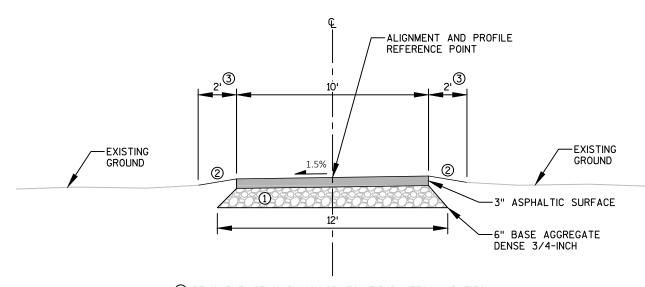


# TYPICAL EXISTING SECTION

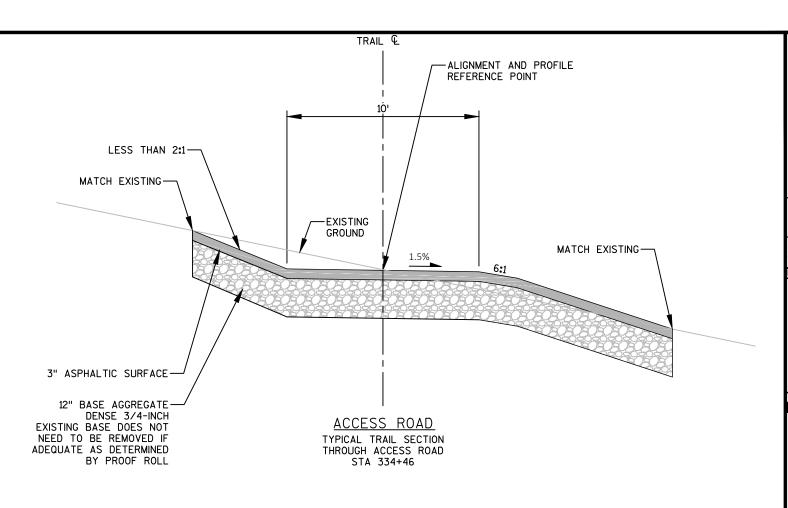
EXISTING RAIL CORRIDOR STA 100+36 TO STA 162+89 STA 200+00 TO STA 211+46 STA 300+00 TO STA 357+75 STA 406+27.69 TO STA 840+00

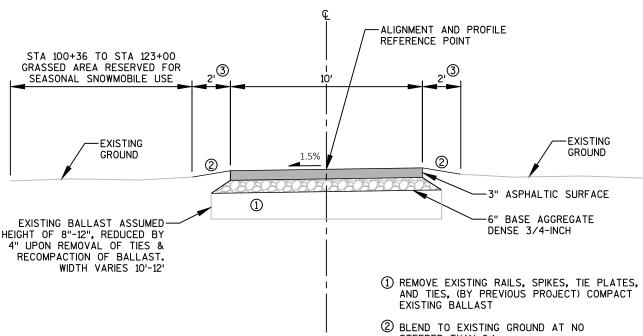


- 1) REMOVE EXISTING RAILS, SPIKES, TIE PLATES, AND TIES (BY PREVIOUS PROJECT). PROOF ROLL REMAINING SUBGRADE TO CONFIRM STABILITY, IF UNSTABLE CORE OUT EXISTING SUBGRADE FOR INSTALLATION OF 6" BASE AGGREGATE DENSE 3/4-INCH AND 3" HMA WITH FINAL SURFACE AT TOP OF RAILROAD TIE ELEVATION
- (2) BLEND TO EXISTING GROUND AT NO STEEPER THAN 6:1
- (3) TOPSOIL, SEED, FERTILIZE & MULCH DISTURBED AREA, MINIMUM 6" TOPSOIL

### TYPICAL FINISHED SECTION

TRAIL WHERE BALLAST IS NO LONGER PRESENT STA 100+42 TO STA 107+15 STA 201+35 TO 201+72 STA 478+33.33 TO STA 546+70





- STEEPER THAN 6:1
- 3 TOPSOIL, SEED, FERTILIZE & MULCH DISTURBED AREA. MINIMUM 6" TOPSOIL

### TYPICAL FINISHED SECTION

TRAIL WITH GRASS SHOULDERS STA 107+15 TO STA 162+89 STA 200+00 TO 201+35 STA 207+50 TO 211+46 STA 300+00 TO STA 308+00

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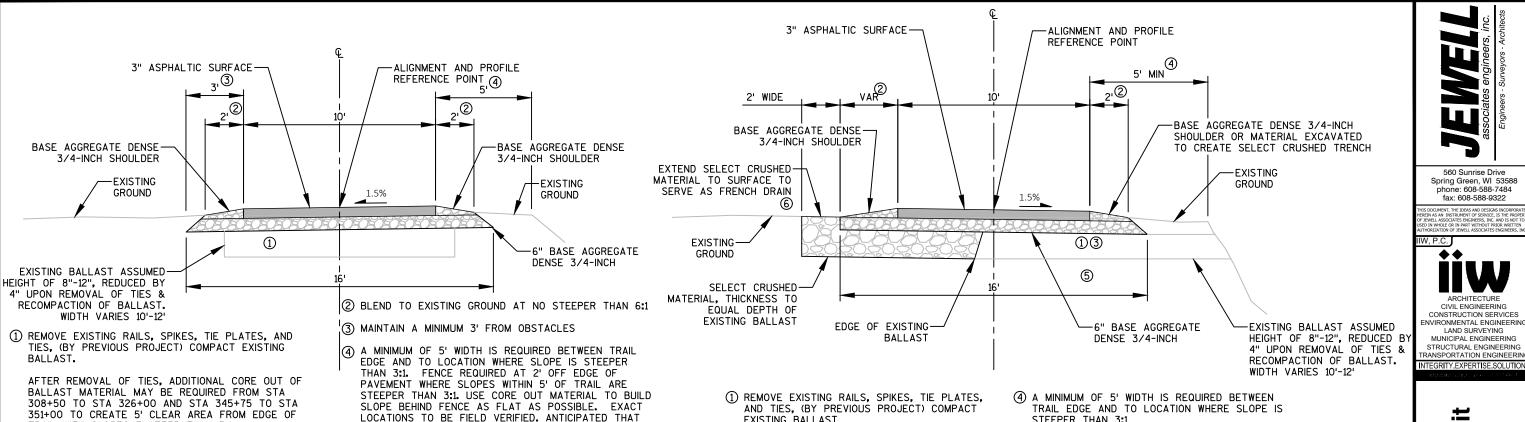
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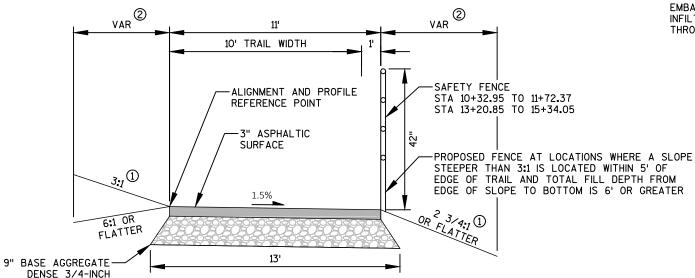
Drawing Name Typical Sections

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# TYPICAL FINISHED SECTION

TRAIL WITH GRANULAR SHOULDERS STA 308+00 TO STA 326+00 STA 343+00 TO STA 357+75



FENCE WILL BE REQUIRED STA 320+80 TO STA 326+00

- 1 SLOPE VARIES, SEE CROSS SECTIONS 3:1 TYP, 2.75:1 OR FLATTER
- (2) TOPSOIL, HYDROSEEDING (USE SEEDING MIXTURE NO. 20, FERTILIZER TYPE-B, AND SEEDING TEMPORARY)
  AND EROSION MAT 1, TYPE B, URBAN CLASS

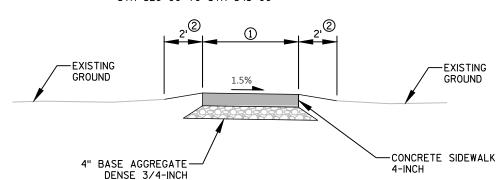
# TYPICAL FINISHED SECTION

WATER STREET SIDEWALK ACCESS TO GREAT SAUK STATE TRAIL AT NORTH EAGLE GATE LOCATION STA 10+24.53 TO STA 16+02.82

- EXISTING BALLAST
- (2) 2' TYP 3' AT LOCATIONS OF FRENCH DRAIN. SEE 6 BLEND TO EXISTING GROUND AT NO STEEPER THAN 6:1
- DURING CONSTRUCTION, CONFIRM CLEAN AGGREGATE EXTENDS TO THE EDGE OF THE EMBANKMENT TO ALLOW WATER TO INFILTRATE AND PERCOLATE OR FLOW THROUGH FOR DRAINAGE
- STEEPER THAN 3:1
- (5) PRIVATE UTILITIES. FIBER OPTIC. TELEPHONE. ELECTRIC & GAS ARE PRESENT THROUGHOUT
- (6) FRENCH DRAIN ONLY INTENDED FOR USE FROM STA 335+00 LT TO STA 343+00 LT. IN OTHER AREAS CONSTRUCT STANDARD 2' WIDE GRANULAR SHOULDERS

#### TYPICAL FINISHED SECTION

HORIZONTAL ALIGNMENT SHIFTED FROM EXISTING RAILROAD TRACK CENTERLINE, TRAIL WITH GRANULAR SHOULDERS STA 326+00 TO STA 343+00



- (1) WIDTH VARIES. SEE SHEETS C4.4 C4.20 FOR MORE INFORMATION.
- 2 TOPSOIL, SEED, FERTILIZE & MULCH DISTURBED AREA, BLEND INTO ADJACENT GROUND

#### TYPICAL FINISHED SECTION

SIDEWALK REPLACEMENT AT CROSSING LOCATIONS

STA 9+76 (WATER STREET ACCESS STA 138+90 RT STA 139+50 LT STA 119+60 STA 139+75 RT STA 123+30 STA 144+55 STA 124+16 STA 150+55 STA 133+75 STA 154+75

STA 157+45 STA 158+08 STA 201+75 STA 206+15 LT STA 211+39 LT STA 304+46 LT

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CIVIL ENGINEERING

LAND SURVEYING

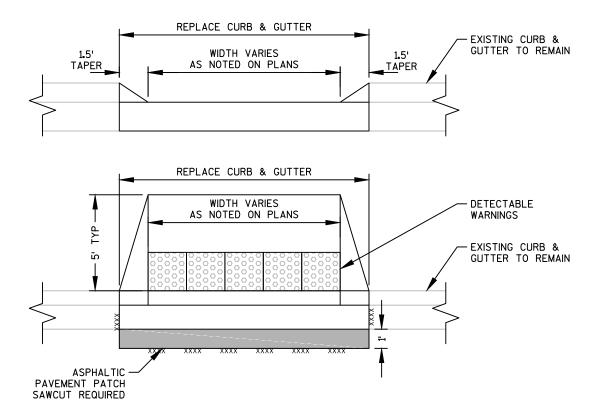
MUNICIPAL ENGINEERING

Drawing Name Typical Sections

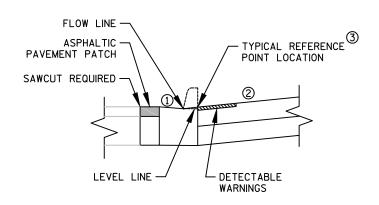
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TRAIL WITH SLOPES FLATTER THAN 3:1

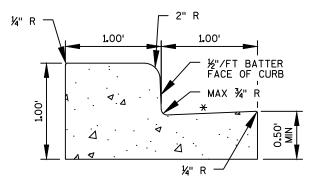


### TYPICAL CURB REPLACEMENT DETAIL



- 1 4% MAXIMUM GUTTER FLAG SLOPE. VARY SLOPE AS REQUIRED TO MATCH EXISTING ROADWAY, GRADE CHANGE BETWEEN SLOPE () AND SLOPE () NOT TO EXCEED 11%
- 2 8.33% MAXIMUM SLOPE, GRADE CHANGE BETWEEN SLOPE 1 AND SLOPE 2 NOT TO EXCEED 11%
- 3 REFERENCE POINT LOCATION AS SHOWN ON CONSTRUCTION DETAIL SHEETS C4.4 - C4.20 IS 0.035' ABOVE GUTTER FLOW LINE

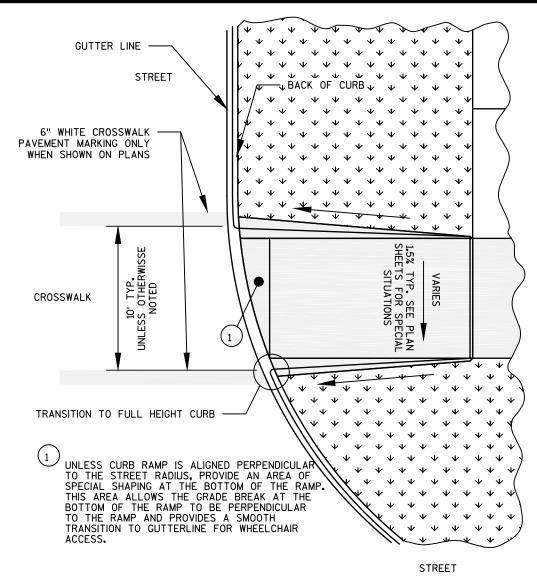
TYPICAL SIDEWALK DROP CURB DETAIL



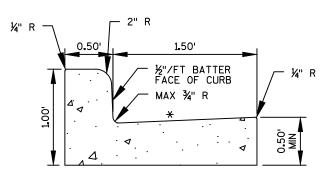
SEE S.D.D. "CONCRETE CURB, CONCRETE CURB & GUTTER AND TIES" FOR ADDITIONAL INFORMATION

\*SLOPE VARIES TO MATCH ADJACENT PAVEMENT OR CURB AS APPROPRIATE. FIELD VERIFY NECESSARY SLOPE, SEE ADDITIONAL SIDEWALK DROP CURB DETAIL FOR RESTRICTIONS AT SIDEWALK CROSSING LOCATIONS.

CONCRETE CURB AND GUTTER 24-INCH TYPE D SPECIAL - PHILLIPS BLVD/US-12



# TYPICAL CROSSWALK MARKING DETAIL



SEE S.D.D. "CONCRETE CURB, CONCRETE CURB & GUTTER AND TIES" FOR ADDITIONAL INFORMATION

\*SLOPE VARIES TO MATCH ADJACENT PAVEMENT OR CURB AS APPROPRIATE. FIELD VERIFY NECESSARY SLOPE. SEE ADDITIONAL SIDEWALK DROP CURB DETAIL FOR RESTRICTIONS AT SIDEWALK CROSSING LOCATIONS.

CONCRETE CURB AND GUTTER 24-INCH TYPE D

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6/14/2017

Drawing Name Construction Details Crosswalk &

**Curb Information** 

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