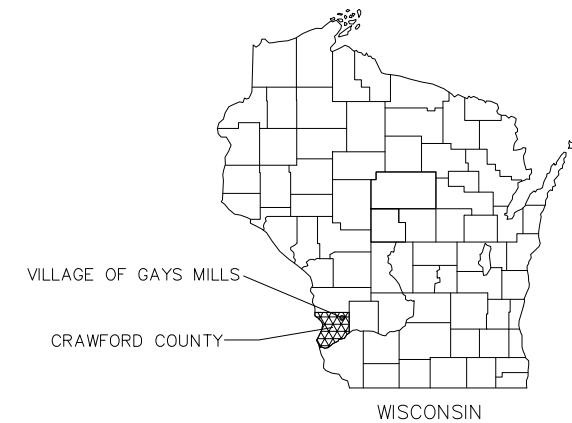


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EROSION CONTROL MEASURES

1. EROSION CONTROL SHALL BE IN ACCORDANCE WITH THE CITY OF VIROQUA EROSION CONTROL ORDINANCE AND CHAPTER NR 216 OF THE WISCONSIN ADMINISTRATIVE CODE.
2. CONSTRUCT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH WISCONSIN DNR TECHNICAL STANDARDS (<http://dnr.wi.gov/runoff/stormwater/techstds.htm>) AND WISCONSIN CONSTRUCTION SITE BEST MANAGEMENT PRACTICE HANDBOOK.
3. INSTALL SEDIMENT CONTROL PRACTICES (TRACKING PAD, PERIMETER SILT FENCE, SEDIMENT BASINS, ETC.) PRIOR TO INITIATING OTHER LAND DISTURBING CONSTRUCTION ACTIVITIES.
4. THE CONTRACTOR IS REQUIRED TO MAKE EROSION CONTROL INSPECTIONS AT THE END OF EACH WEEK AND WHEN 0.5 INCHES OF RAIN FALLS WITHIN 24 HOURS. INSPECTION REPORTS SHALL BE PREPARED AND FILED AS REQUIRED BY THE DNR AND CITY. ALL MAINTENANCE WILL FOLLOW AN INSPECTION WITHIN 24 HOURS.
5. EROSION CONTROL IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ACCEPTANCE OF THIS PROJECT. EROSION CONTROL MEASURES AS SHOWN SHALL BE THE MINIMUM PRECAUTIONS THAT WILL BE ALLOWED. ADDITIONAL EROSION CONTROL MEASURES, AS REQUESTED IN WRITING BY THE STATE OR LOCAL INSPECTORS, OR THE ENGINEER, SHALL BE INSTALLED WITHIN 24 HOURS.
6. A 3" CLEAR STONE TRACKING PAD SHALL BE INSTALLED AT THE END OF ROAD CONSTRUCTION LIMITS TO PREVENT SEDIMENT FROM BEING TRACKED ONTO THE ADJACENT PAVED PUBLIC ROADWAY. SEDIMENT TRACKING PAD SHALL CONFORM TO WisDNR TECHNICAL STANDARD 1057. SEDIMENT REACHING THE PUBLIC ROAD SHALL BE REMOVED BY STREET CLEANING (NOT HYDRAULIC FLUSHING) BEFORE THE END OF EACH WORK DAY.
7. CHANNELIZED RUNOFF: FROM ADJACENT AREAS PASSING THROUGH THE SITE SHALL BE DIVERTED AROUND DISTURBED AREAS.
8. STABILIZED DISTURBED GROUND: ANY SOIL OR DIRT PILES WHICH WILL REMAIN IN EXISTENCE FOR MORE THAN 7-CONSECUTIVE DAYS, WHETHER TO BE WORKED DURING THAT PERIOD OR NOT, SHALL NOT BE LOCATED WITHIN 25-FEET OF ANY ROADWAY, PARKING LOT, PAVED AREA, OR DRAINAGE STRUCTURE OR CHANNEL (UNLESS INTENDED TO BE USED AS PART OF THE EROSION CONTROL MEASURES). TEMPORARY STABILIZATION AND CONTROL MEASURES (SEEDING, MULCHING, TARPING, EROSION MATTING, BARRIER FENCING, ETC.) ARE REQUIRED FOR THE PROTECTION OF DISTURBED AREAS AND SOIL PILES, WHICH WILL REMAIN UN-WORKED FOR A PERIOD OF MORE THAN 14-CONSECUTIVE CALENDAR DAYS. THESE MEASURES SHALL REMAIN IN PLACE UNTIL SITE HAS STABILIZED.
9. SITE DE-WATERING: WATER PUMPED FROM THE SITE SHALL BE TREATED BY TEMPORARY SEDIMENTATION BASINS OR OTHER APPROPRIATE CONTROL MEASURES. SEDIMENTATION BASINS SHALL HAVE A DEPTH OF AT LEAST 3 FEET, BE SURROUNDED BY SNOWFENCE OR EQUIVALENT BARRIER AND HAVE SUFFICIENT SURFACE AREA TO PROVIDE A SURFACE SETTLING RATE OF NO MORE THAN 750 GALLONS PER SQUARE FOOT PER DAY AT THE HIGHEST DEWATERING PUMPING RATE. WATER MAY NOT BE DISCHARGED IN A MANNER THAT CAUSES EROSION OF THE SITE, A NEIGHBORING SITE, OR THE BED OR BANKS OF THE RECEIVING WATER. POLYMERS MAY BE USED AS DIRECTED BY DNR TECHNICAL STANDARD 1061 (DE-WATERING).
10. WASHED STONE WEEPERS OR TEMPORARY EARTH BERMS SHALL BE BUILT PER PLAN BY CONTRACTOR TO TRAP SEDIMENT OR SLOW THE VELOCITY OF STORM WATER.
11. SEE DETAIL SHEETS FOR RIP-RAP SIZING. IN NO CASE WILL RIP-RAP BE SMALLER THAN 3" TO 6".
12. INLET FILTERS ARE TO BE PLACED IN STORMWATER INLET STRUCTURES AS SOON AS THEY ARE INSTALLED. ALL PROJECT AREA STORM INLETS NEED WISCONSIN D.O.T. TYPE D INLET PROTECTION. THE FILTERS SHALL BE MAINTAINED UNTIL THE CITY HAS ACCEPTED THE BINDER COURSE OF ASPHALT.
13. USE DETENTION BASINS AS SEDIMENT BASINS DURING CONSTRUCTION (DO NOT USE INFILTRATION AREAS). AT THE END OF CONSTRUCTION, REMOVE SEDIMENT AND RESTORE PER PLAN.
14. RESTORATION (SEED, FERTILIZE AND MULCH) SHALL BE PER SPECIFICATIONS ON THIS SHEET (NOTE: ADD SEEDING RATE STANDARD OF DETAIL BLOCK TO PLAN) UNLESS SPECIAL RESTORATION IS CALLED FOR ON THE LANDSCAPE PLAN OR THE DETENTION BASIN DETAIL SHEET.
15. TERRACES SHALL BE RESTORED WITH 6" TOPSOIL, PERMANENT SEED, FERTILIZER AND MULCH. LOTS SHALL BE RESTORED WITH 6" TOPSOIL, TEMPORARY SEED, FERTILIZER AND MULCH.
16. AFTER DETENTION BASIN GRADING IS COMPLETE, THE BOTTOM OF DRY BASINS SHALL RECEIVE 6" TOPSOIL AND SHALL BE CHISEL-PLOWED TO A MINIMUM DEPTH OF 12" PRIOR TO RESTORATION.
17. SEED, FERTILIZER AND MULCH SHALL BE APPLIED WITHIN 7 DAYS AFTER FINAL GRADE HAS BEEN ESTABLISHED. IF DISTURBED AREAS WILL NOT BE RESTORED IMMEDIATELY AFTER ROUGH GRADING, TEMPORARY SEED SHALL BE PLACED.
18. FOR THE FIRST SIX WEEKS AFTER RESTORATION (E.G. SEED & MULCH, EROSION MAT, SOD) OF A DISTURBED AREA, INCLUDE SUMMER WATERING PROVISIONS OF ALL NEWLY SEEDED AND MULCHED AREAS WHENEVER 7 DAYS ELAPSE WITHOUT A RAIN EVENT.
19. EROSION MAT (TYPE I CLASS A PER WISCONSIN D.O.T. P.A.L.) SHALL BE INSTALLED ON ALL SLOPES 3:1 OR GREATER BUT LESS THAN 1:1.
20. SILT FENCE OR EROSION MAT SHALL BE INSTALLED ALONG THE CONTOURS AT 100 FOOT INTERVALS DOWN THE SLOPE ON THE DISTURBED SLOPES STEEPER THAN 5% AND MORE THAN 100 FEET LONG THAT SHEET FLOW TO THE ROADWAY UNLESS SOIL STABILIZERS ARE USED.
21. SILT FENCE TO BE USED ACROSS AREAS OF THE LOT THAT SLOPE TOWARDS A PUBLIC STREET OR WATERWAY. SEE DETAILS.
22. SEDIMENT SHALL BE CLEANED FROM CURB AND GUTTER AFTER EACH RAINFALL AND PRIOR TO PROJECT ACCEPTANCE.
23. ACCUMULATED CONSTRUCTION SEDIMENT SHALL BE REMOVED FROM ALL PERMANENT BASINS TO THE ELEVATION SHOWN ON THE GRADING PLAN FOLLOWING THE STABILIZATION OF DRAINAGE AREAS.
24. ALL CONSTRUCTION ENTRANCES SHALL HAVE TEMPORARY ROAD CLOSED SIGNS THAT WILL BE IN PLACE WHEN THE ENTRANCE IS NOT IN USE AND AT THE END OF EACH DAY.
25. THE CITY AND/OR ENGINEER MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES AT ANY TIME DURING CONSTRUCTION.

AGENCIES

EMERGENCY – FIRE, RESCUE, AMBULANCE, POLICE
DIAL 911

GAYS MLLS FIRE DEPARTMENT
KEVIN OPPRICHT
GAYS MILLS, WI 54631
(608) 735-4424 NON-EMERGENCY

UNITED STATES POST OFFICE
225 ORIN STREET
GAYS MILLS, WI 54631
(608) 735-4450

CRAWFORD COUNTY – HIGHWAY DEPARTMENT
21515 STH 27
SENECA, WI 54654
(608) 421-8875

CRAWFORD COUNTY – SHERIFF’S DEPARTMENT
224 N. BEAUMONT ROAD
PRAIRIE DU CHIEN, WI 53821
(608) 326-0241 NON-EMERGENCY

WIDOT
MARK GOGGIN
3550 MORMAN COULEE ROAD
LACROSSE, WI 54601-6767
(608) 789-5955

OWNER

VILLAGE OF GAYS MILLS –VILLAGE HALL
16381 STH 131, SUITE #1
GAYS MILLS, WI 54631
ATTN: DAWN McCANN, VILLAGE CLERK
(608) 735-4341

ENGINEER

VIERBICHER ASSOCIATES INC
400 VIKING DRIVE
REEDSBURG, WI 53959
ATTN: CRAIG MATHEWS
(608) 524-6468

UTILITIES

VILLAGE OF GAYS MILLS –SEWER & WATER
16381 STH 131, SUITE #1
GAYS MILLS, WI 54631
ATTN: JIM CHELLEVOLD, DPW
(608) 735-4341

MEDIACOM LLC WISCONSIN – TELEPHONE
115 S MARQUETTE RD
PRAIRIE DU CHIEN, WI 53821
ATTN: TIM ORCUTT
(608) 326-0478
(515) 249-5848

ALLIANT ENERGY – ELECTRIC
761 ENTERPRISE DRIVE
PLATTEVILLE, WI 53818
ATTN: JERRY NICHOLSON
(608) 342-4113
(608) 558-7777 CELL

RICHLAND-GRANT TELEPHONE – TELEPHONE
PO BOX 67
BLUE RIVER, WI 53518
ATTN: JOHN BARTZ
(608) 537-2461

MADISON GAS & ELECTRIC – GAS
PO BOX 109
526 E. DECKER
VIROQUA, WI 54665
ATTN: MARK OLSON
(608) 637-3139



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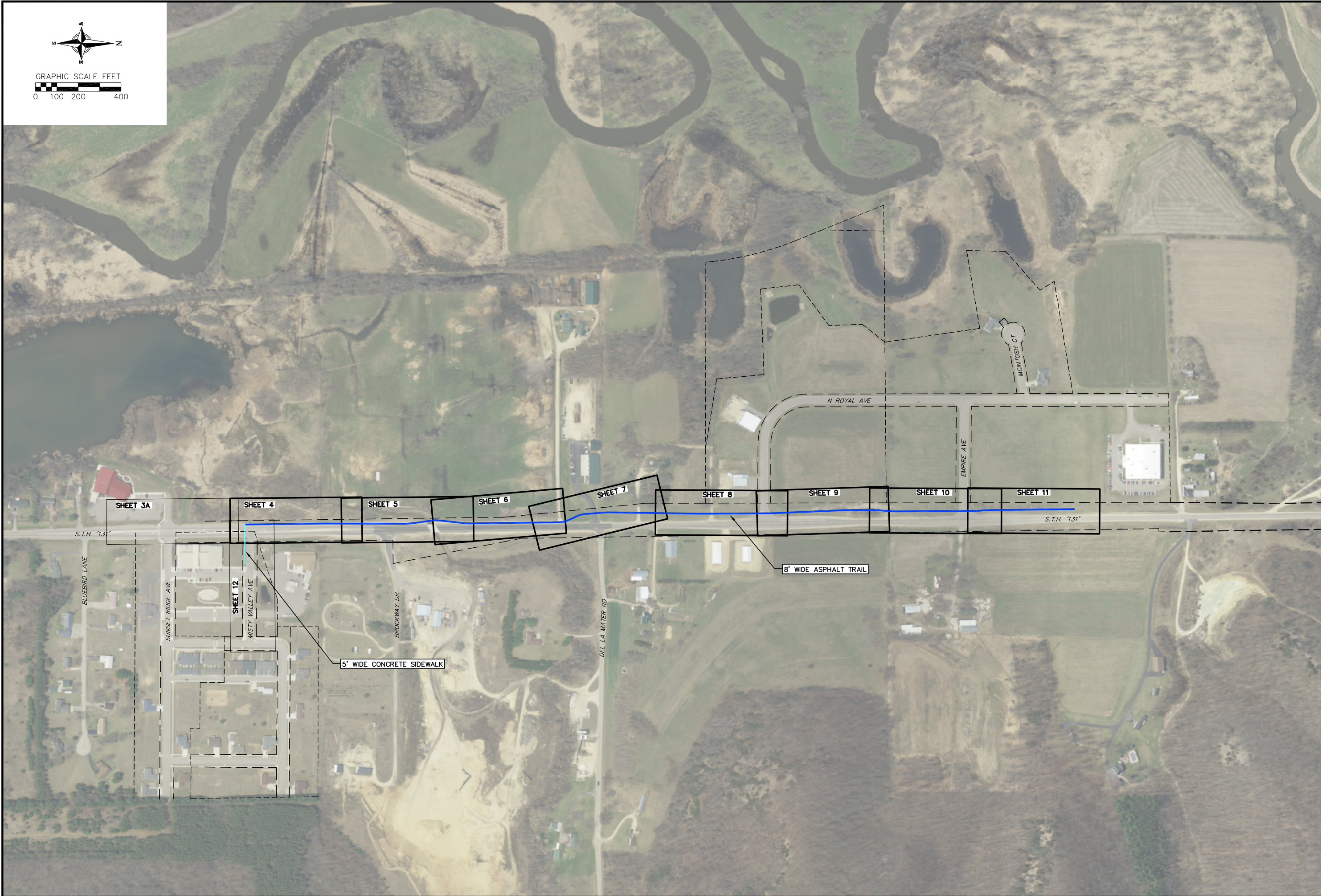
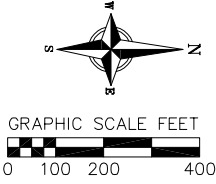
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GENERAL NOTES
2018 TID NO. 1 INFRASTRUCTURE
TRAIL IMPROVEMENTS
GAYS MILLS, WISCONSIN


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OVERALL SHEET INDEX							
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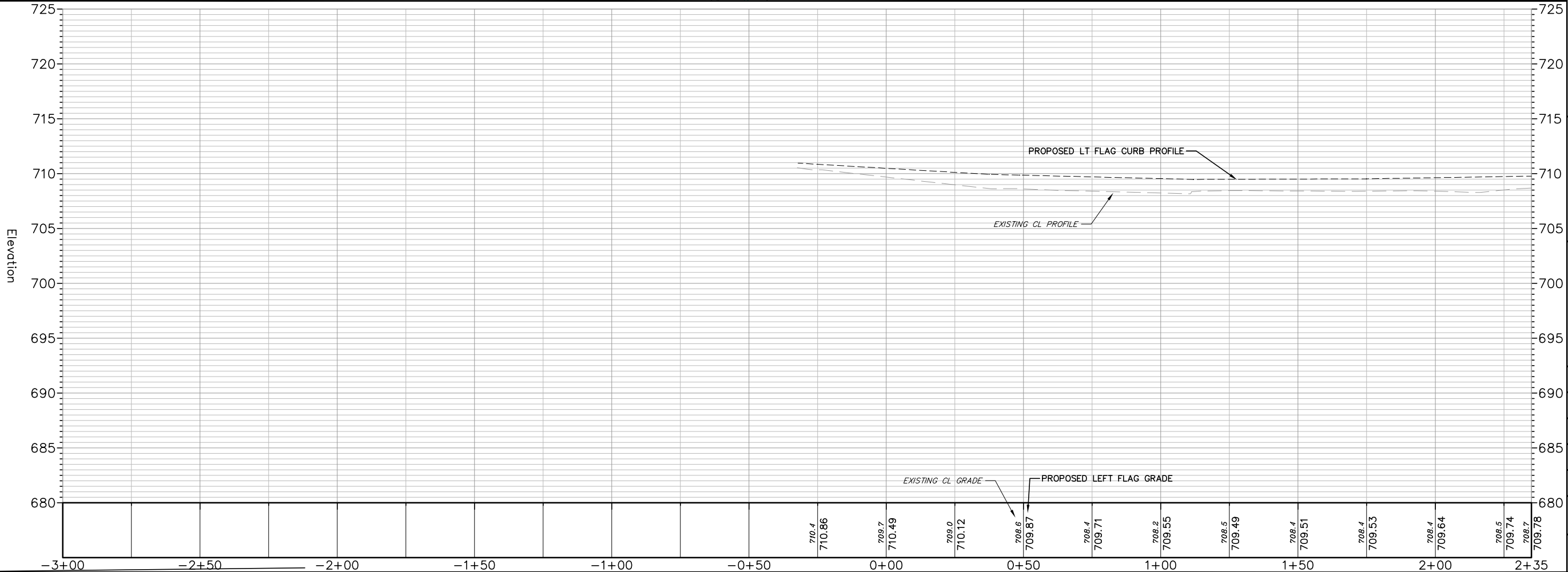
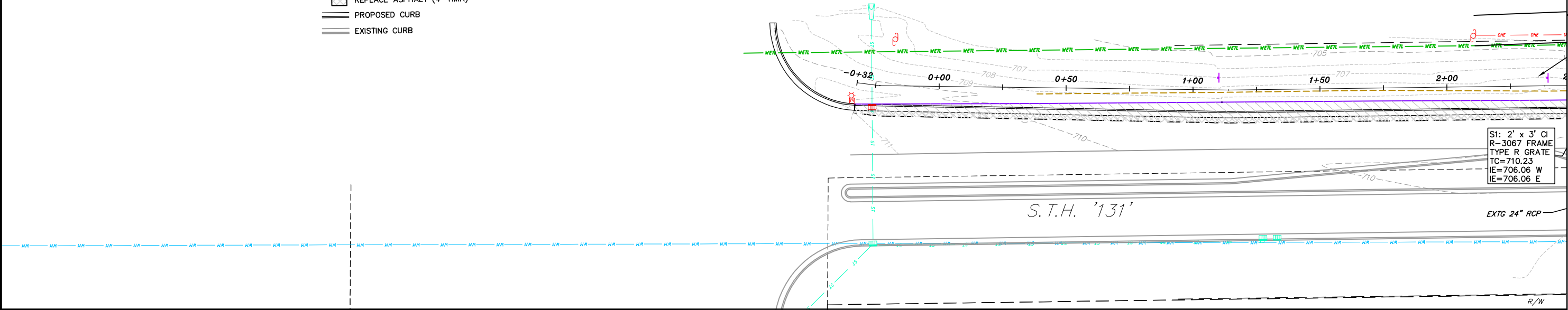
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LEGEND

- NEW ASPHALT TRAIL (2.5")
- NEW CONCRETE (5")
- REMOVE ASPHALT
- REPLACE ASPHALT (4" HMA)
- PROPOSED CURB
- EXISTING CURB



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Consider leaving or moving out - provides positive crosswalk lighting

Move existing W11-2 & W16-9P to Sta. 6+75

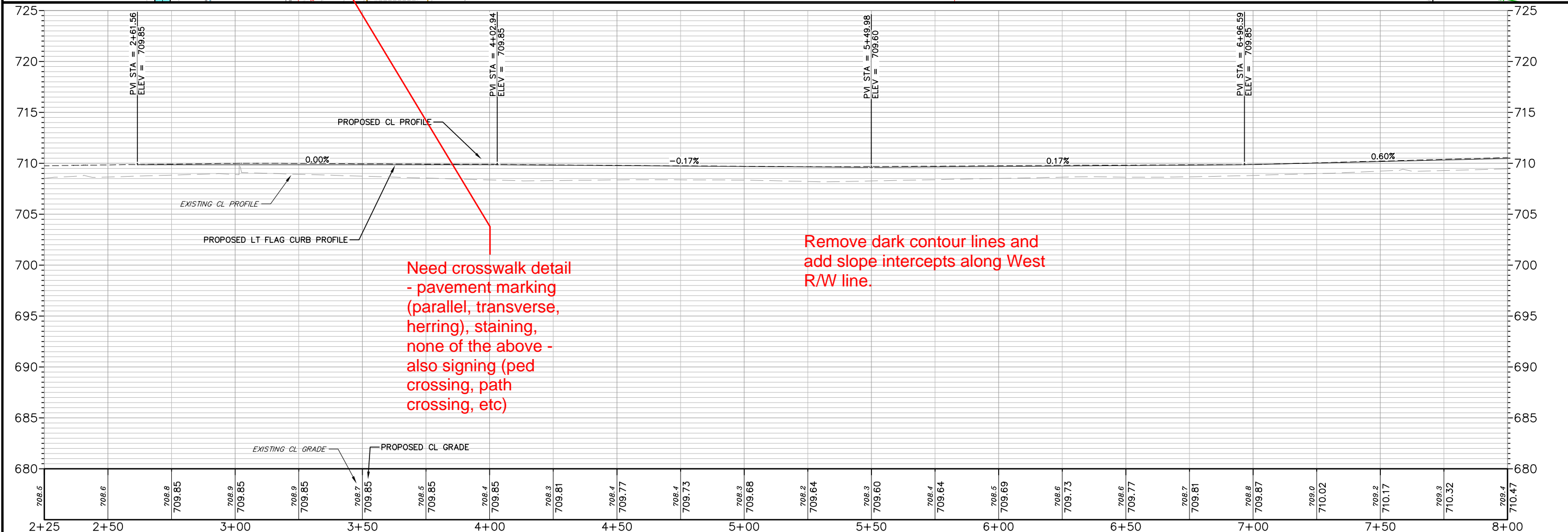
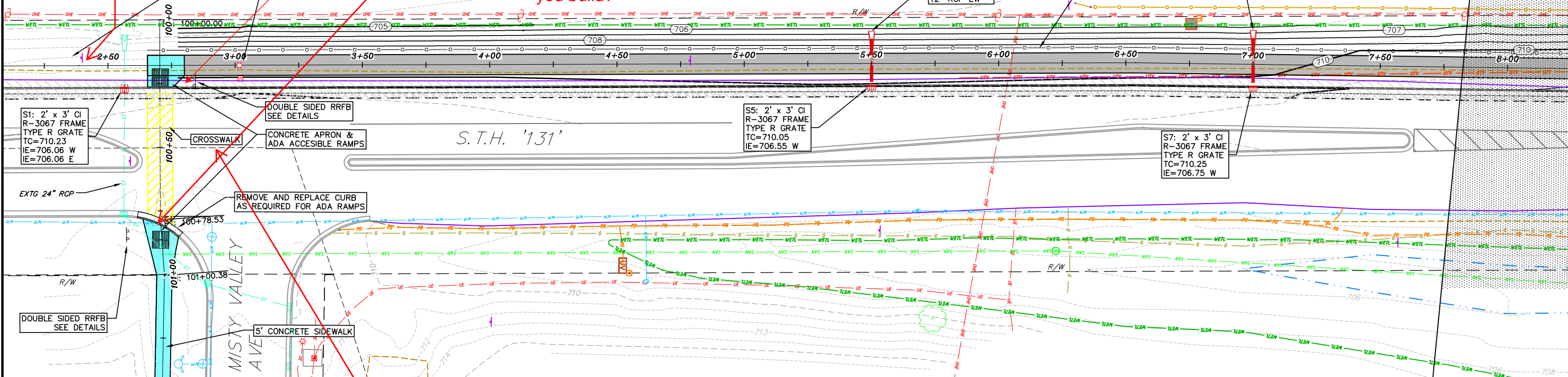
saw cuts? asphalt patch? how construct?

Do we need path to be in reference to WIS 131 C/L? All cross sections have CL of trail, but no alignment information is provided - how will you build?

- LEGEND
- NEW ASPHALT TRAIL (2.5")
 - NEW CONCRETE (5")
 - REMOVE ASPHALT
 - REPLACE ASPHALT (4" HMA)
 - PROPOSED CURB
 - EXISTING CURB



GRAPHIC SCALE FEET
0 10 20 40



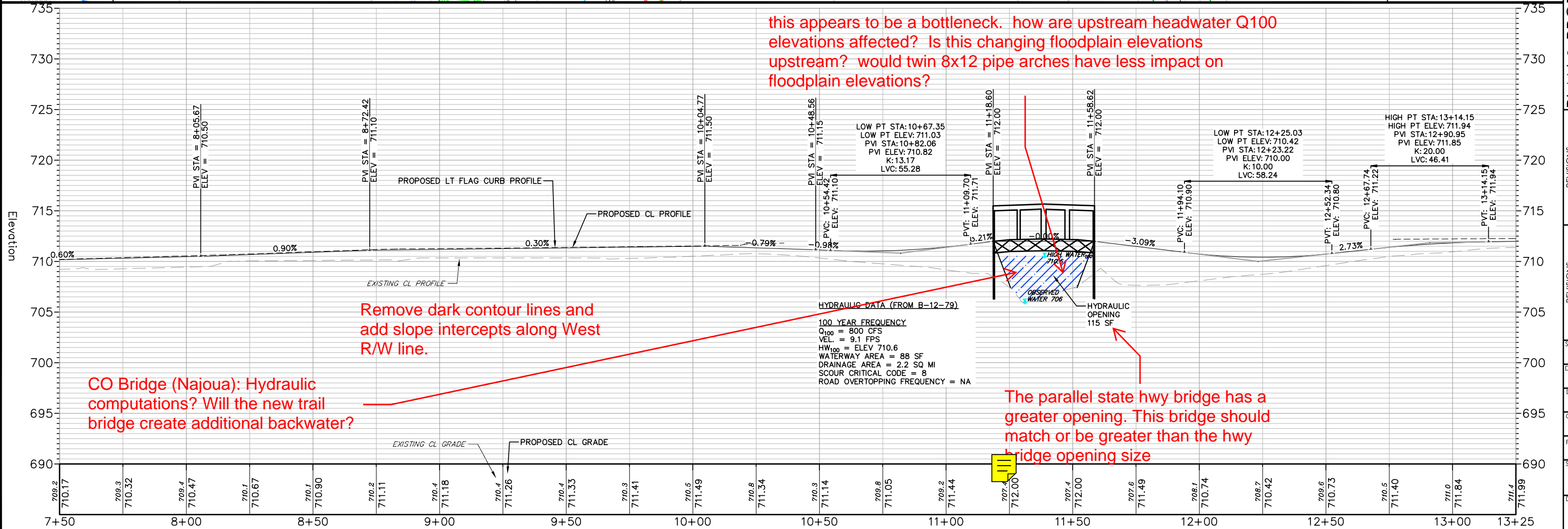
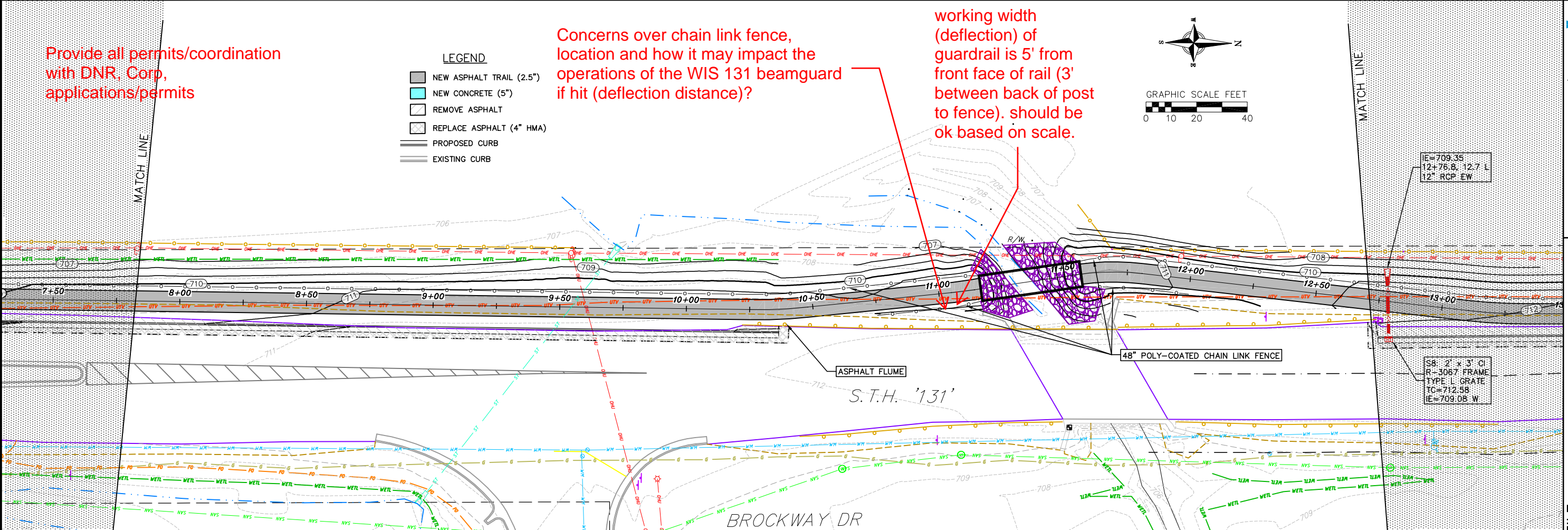
Need crosswalk detail - pavement marking (parallel, transverse, herring), staining, none of the above - also signing (ped crossing, path crossing, etc)

Remove dark contour lines and add slope intercepts along West R/W line.

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Provide all permits/coordination with DNR, Corp, applications/permits

Concerns over chain link fence, location and how it may impact the operations of the WIS 131 beamguard if hit (deflection distance)?

working width (deflection) of guardrail is 5' from front face of rail (3' between back of post to fence). should be ok based on scale.

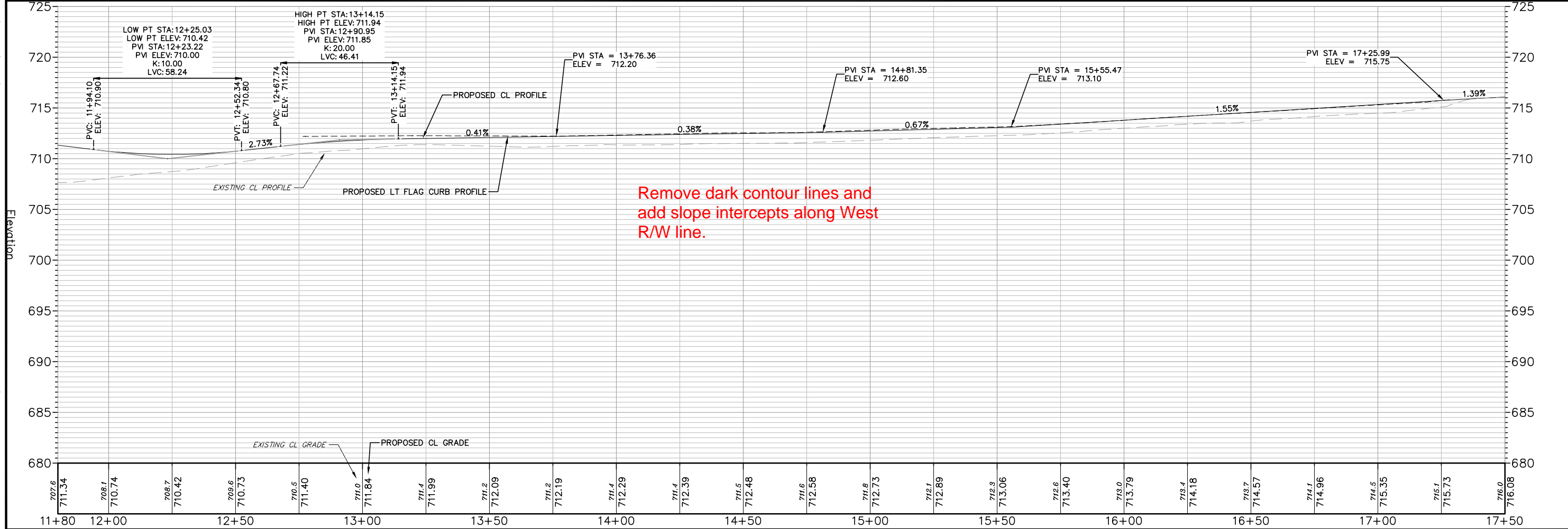
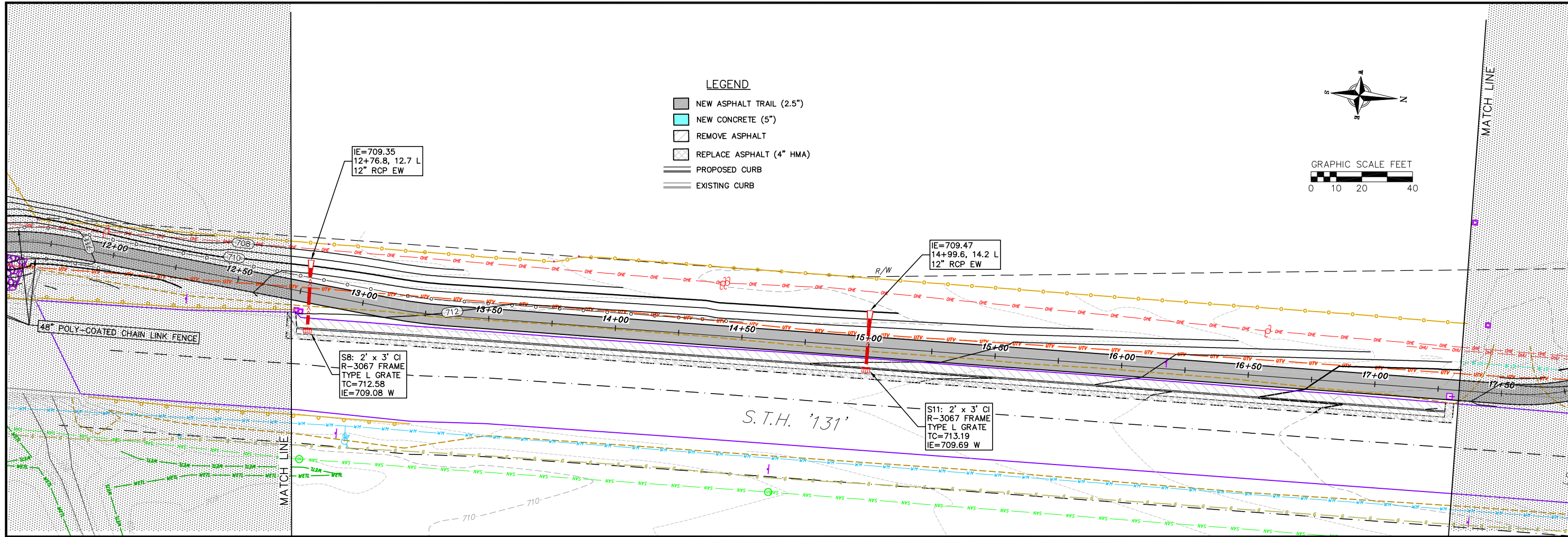
this appears to be a bottleneck. how are upstream headwater Q100 elevations affected? Is this changing floodplain elevations upstream? would twin 8x12 pipe arches have less impact on floodplain elevations?

Remove dark contour lines and add slope intercepts along West R/W line.

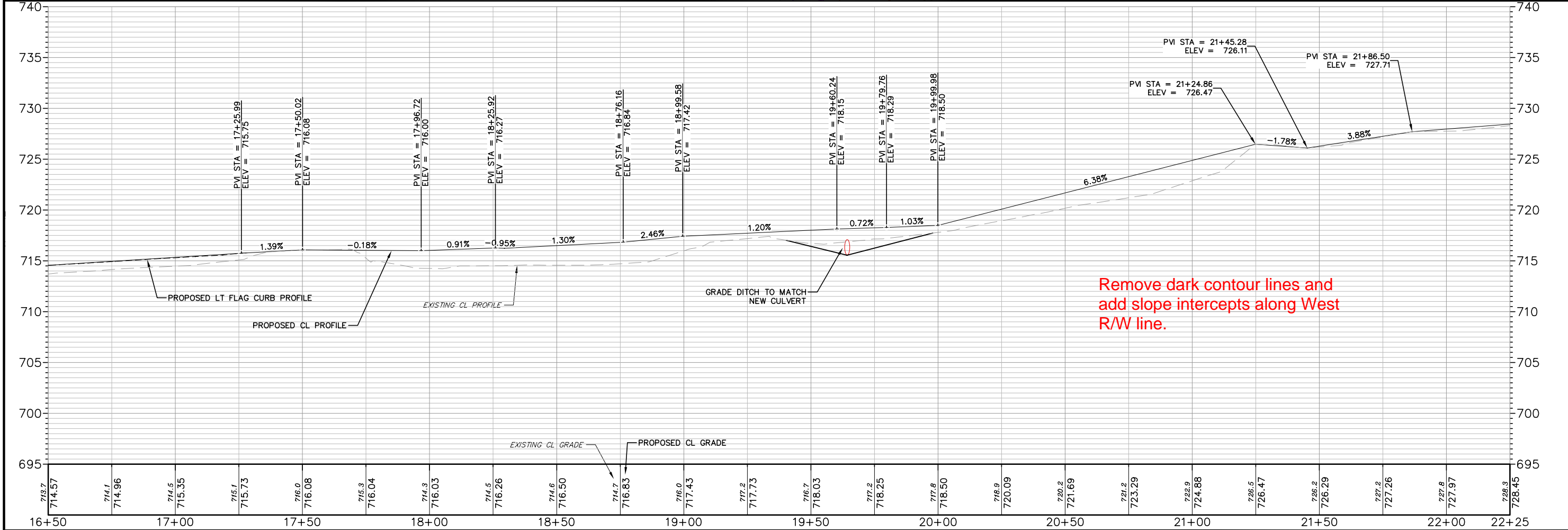
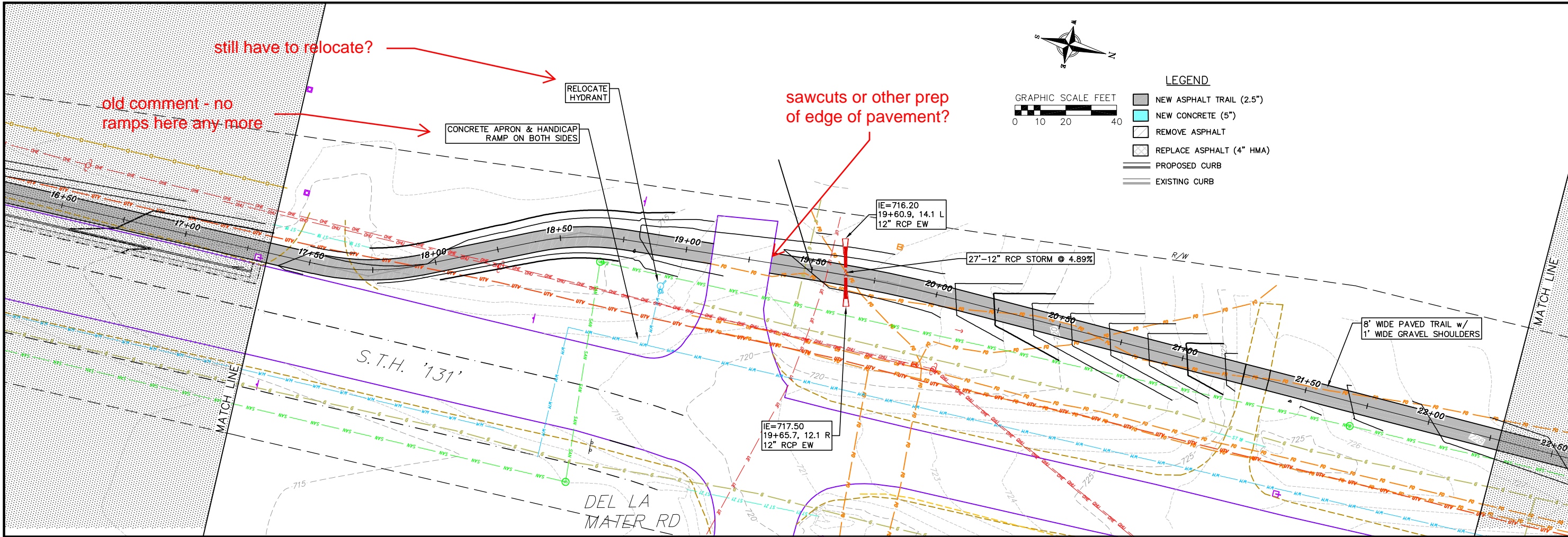
CO Bridge (Najoua): Hydraulic computations? Will the new trail bridge create additional backwater?

The parallel state hwy bridge has a greater opening. This bridge should match or be greater than the hwy bridge opening size

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Remove dark contour lines and add slope intercepts along West R/W line.



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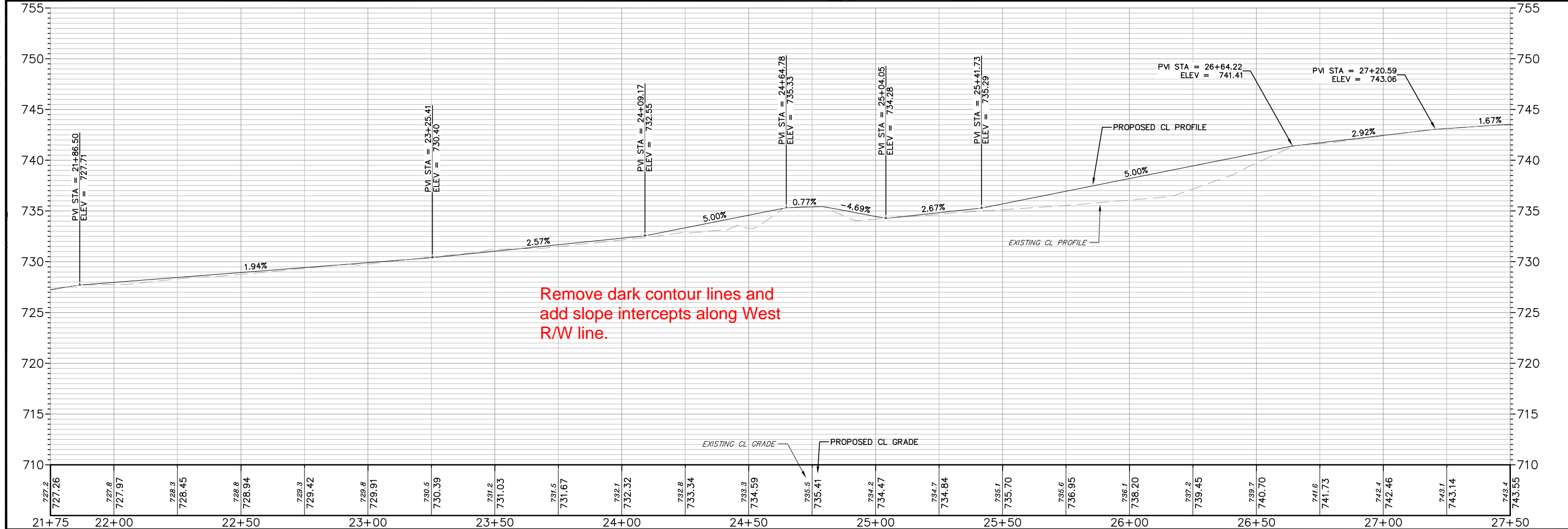
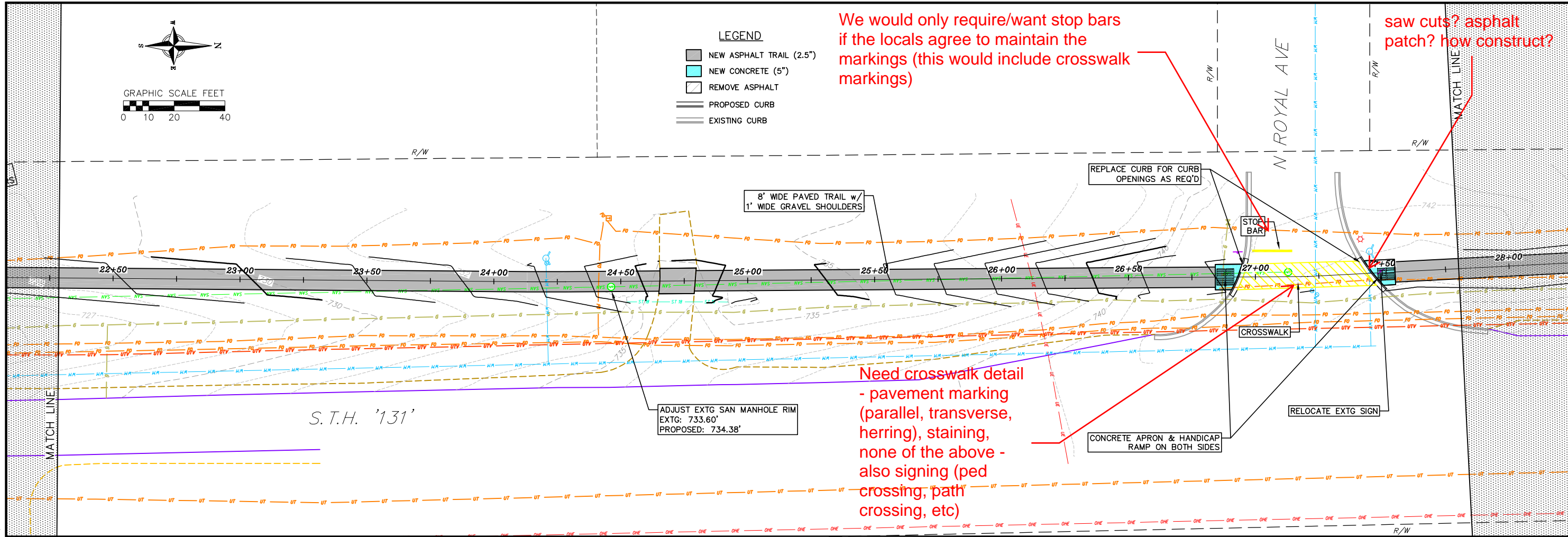
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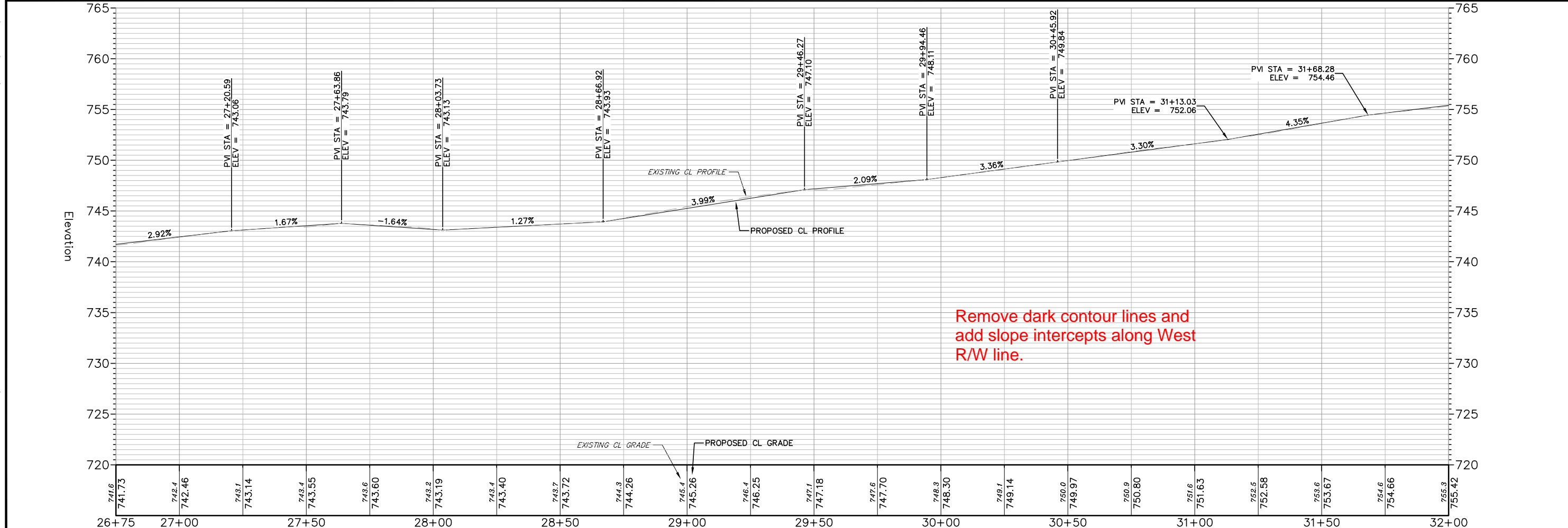
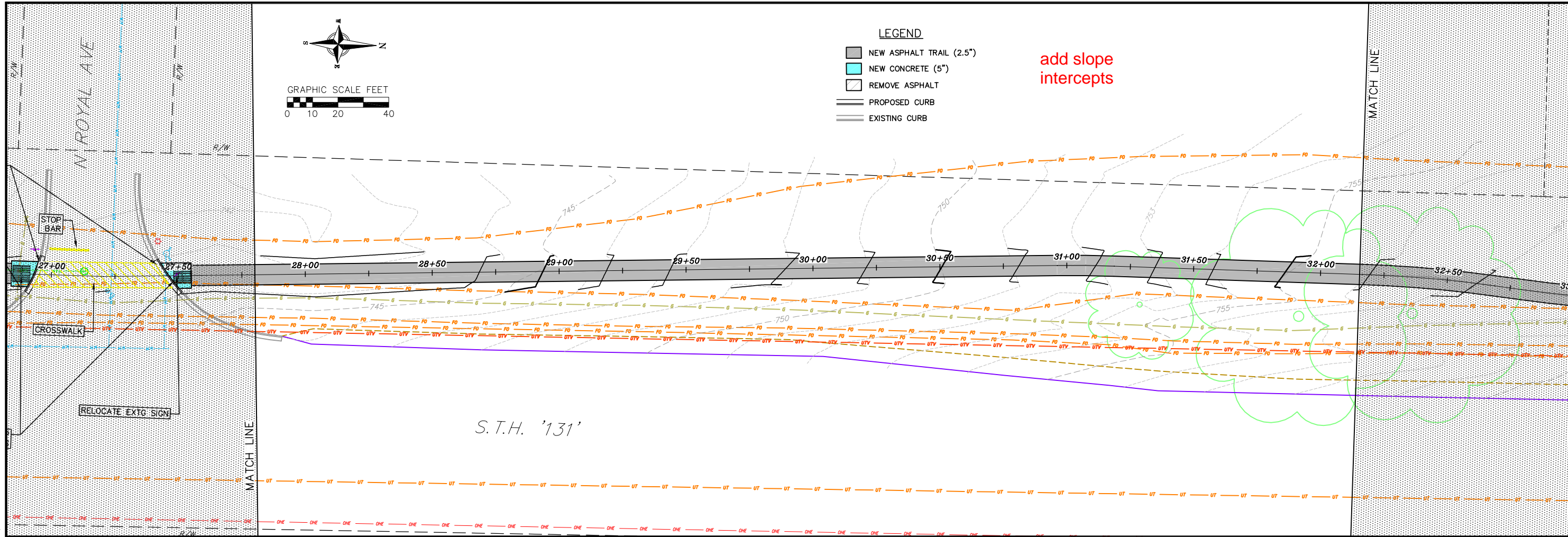
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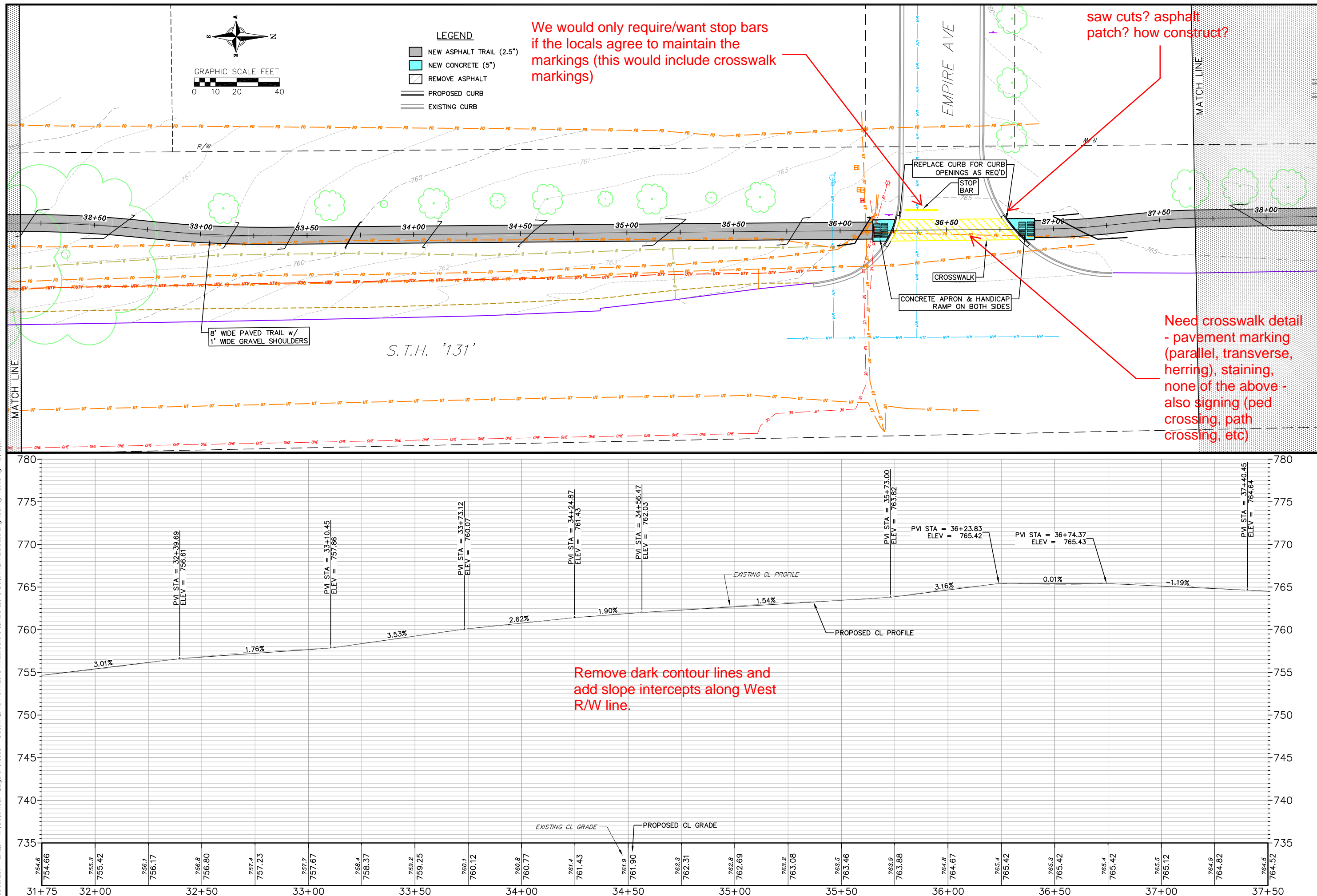
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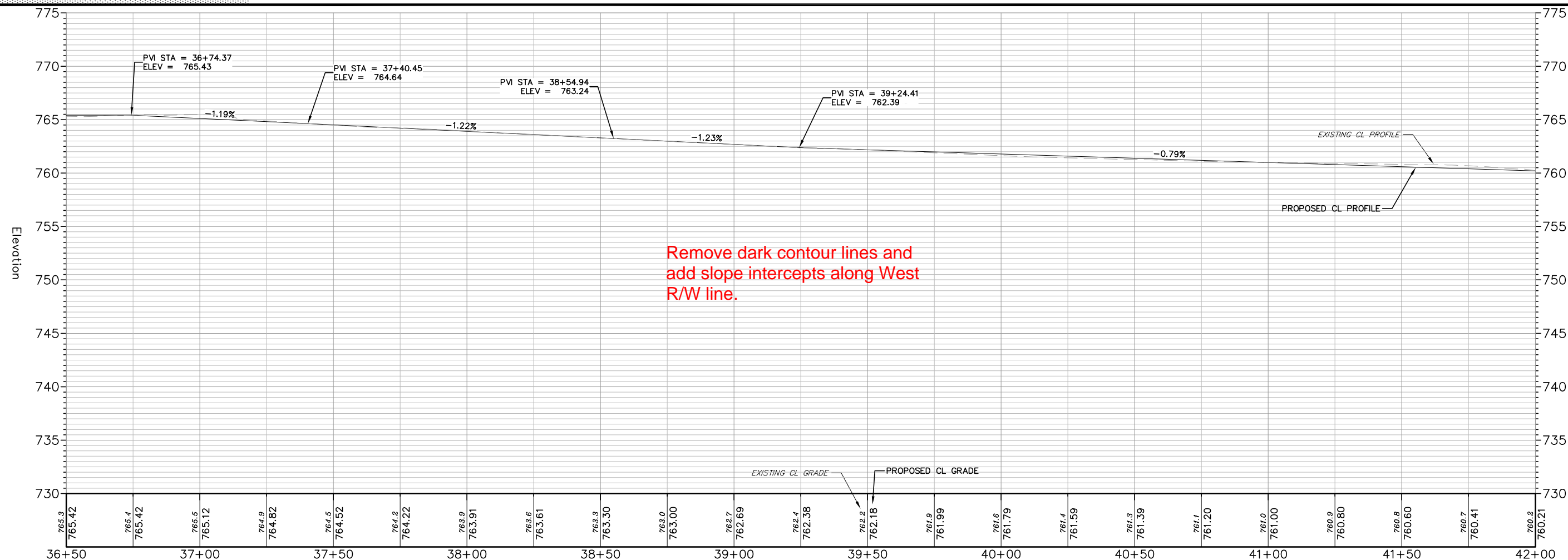
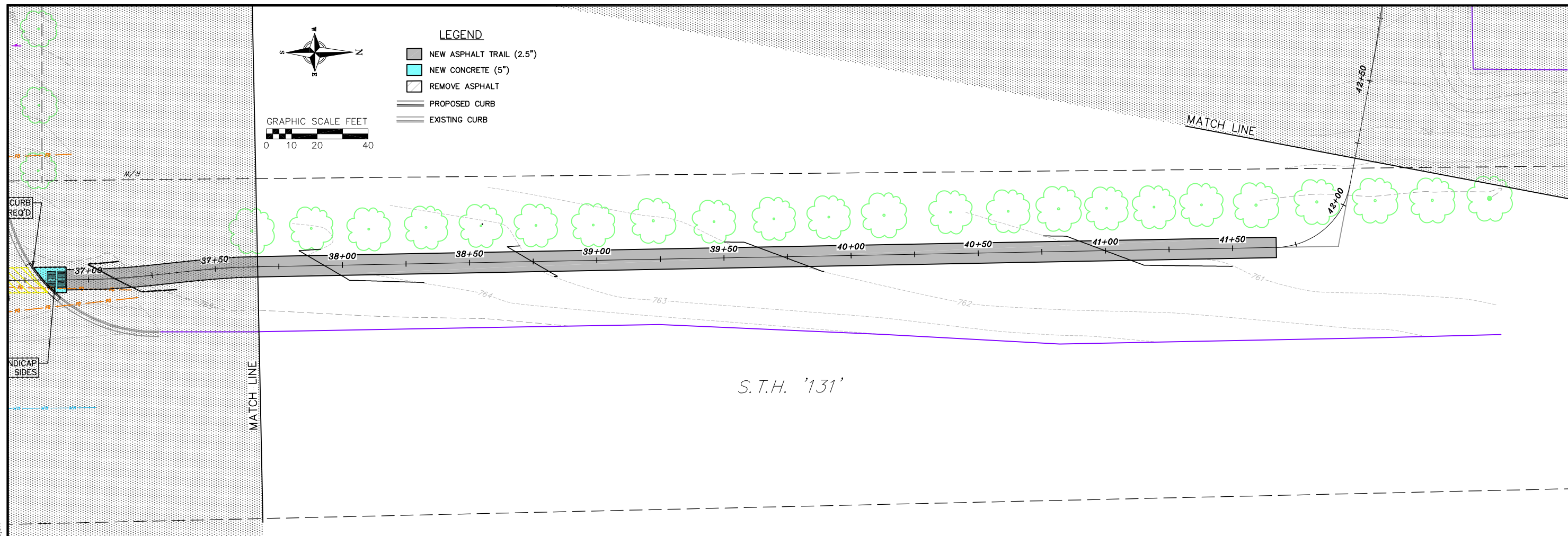
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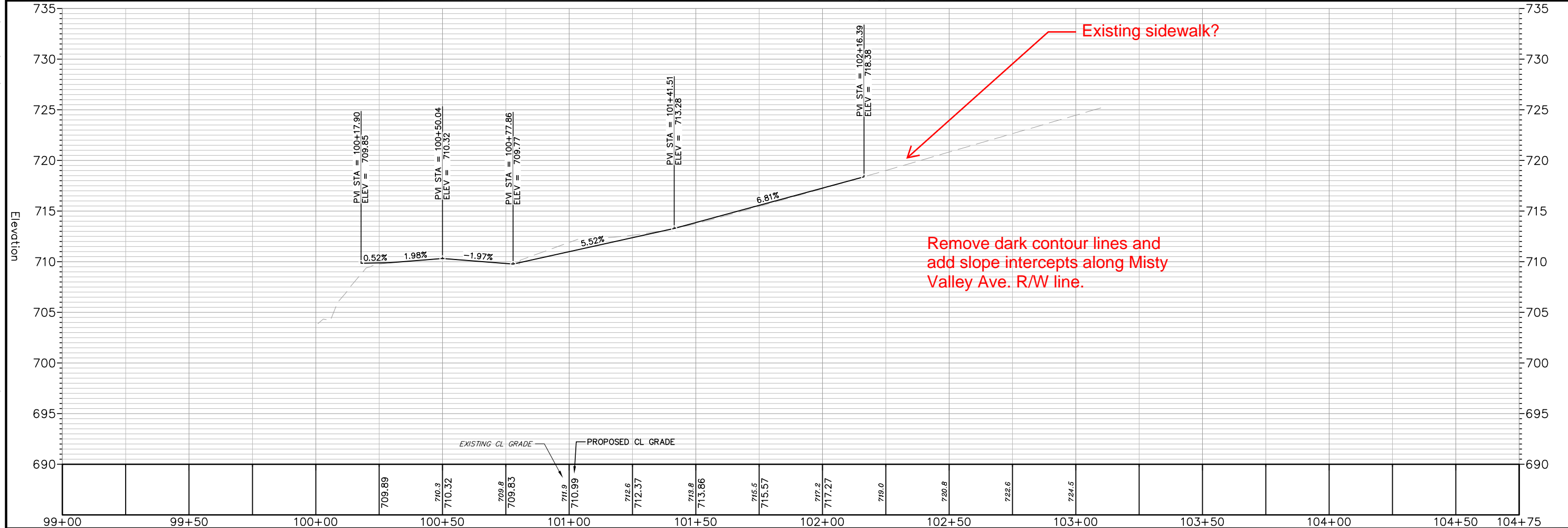
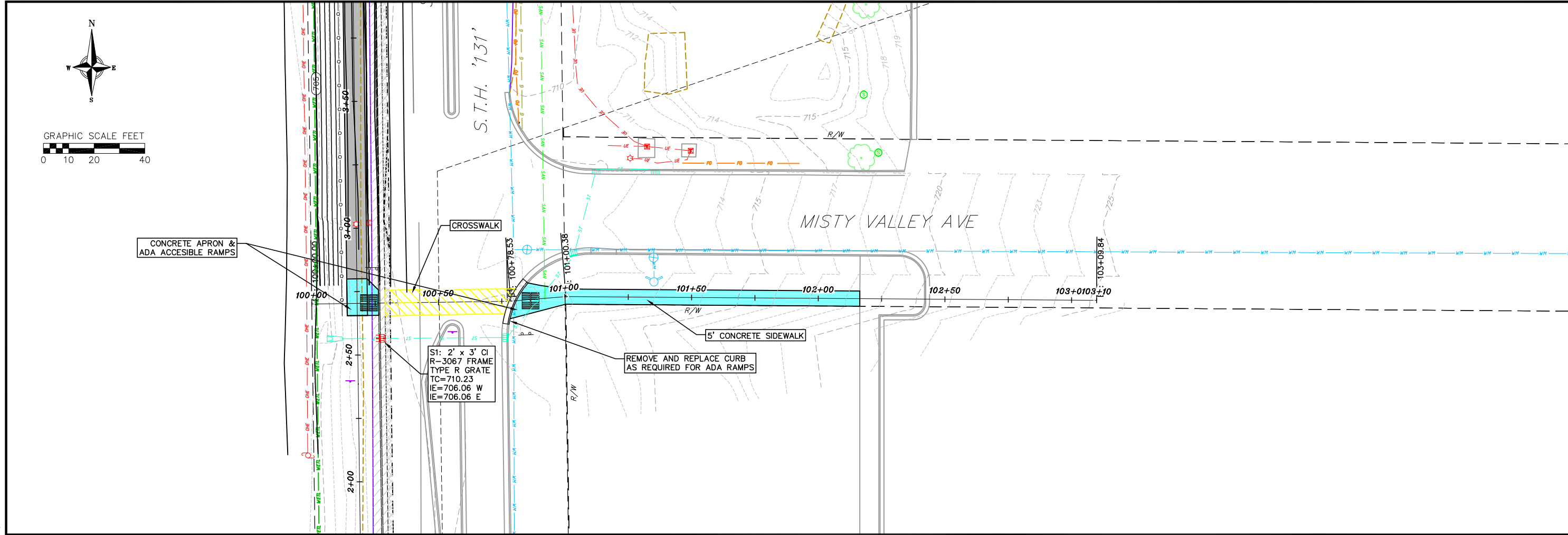
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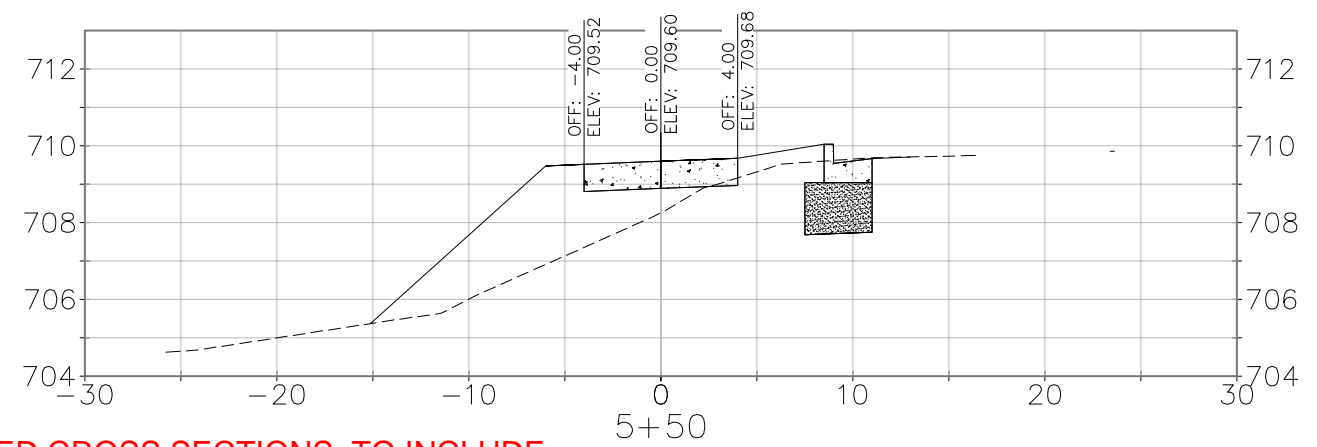
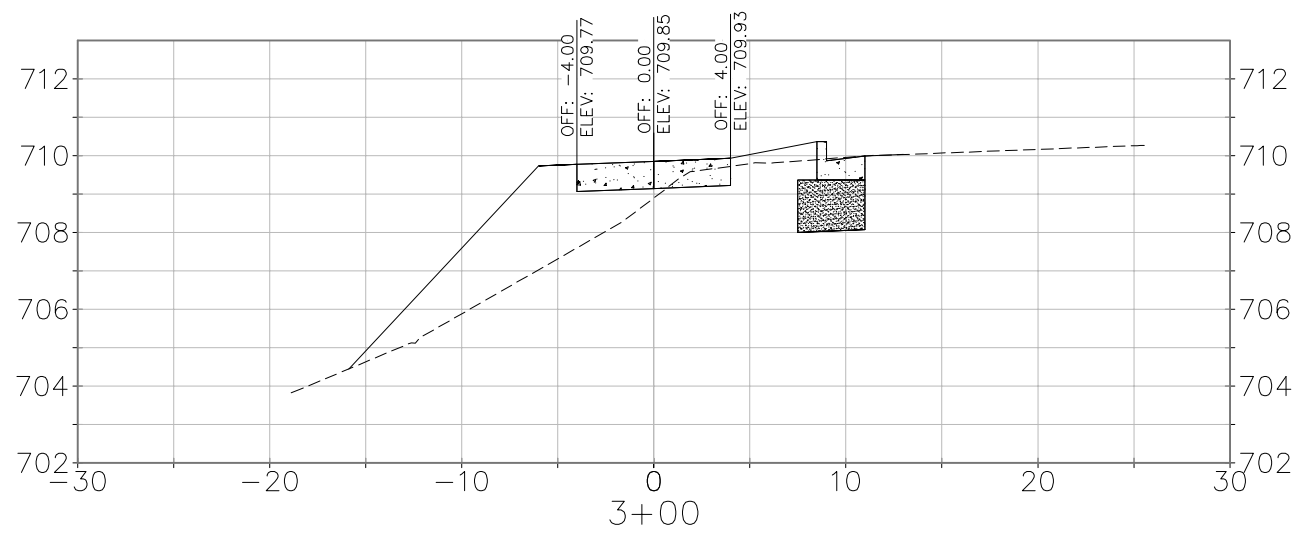
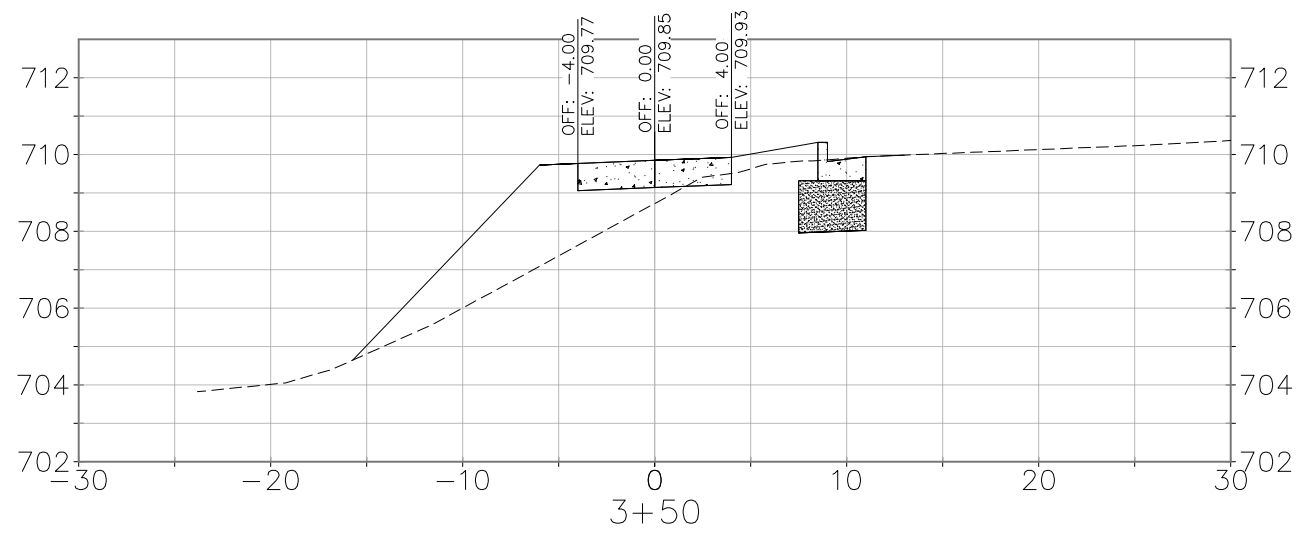
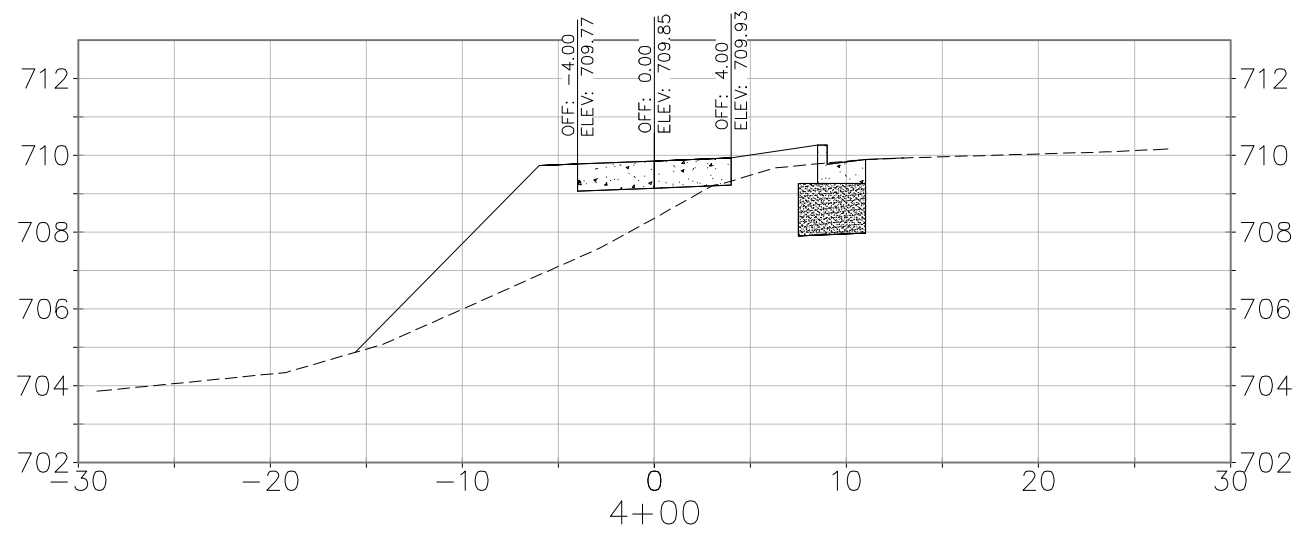




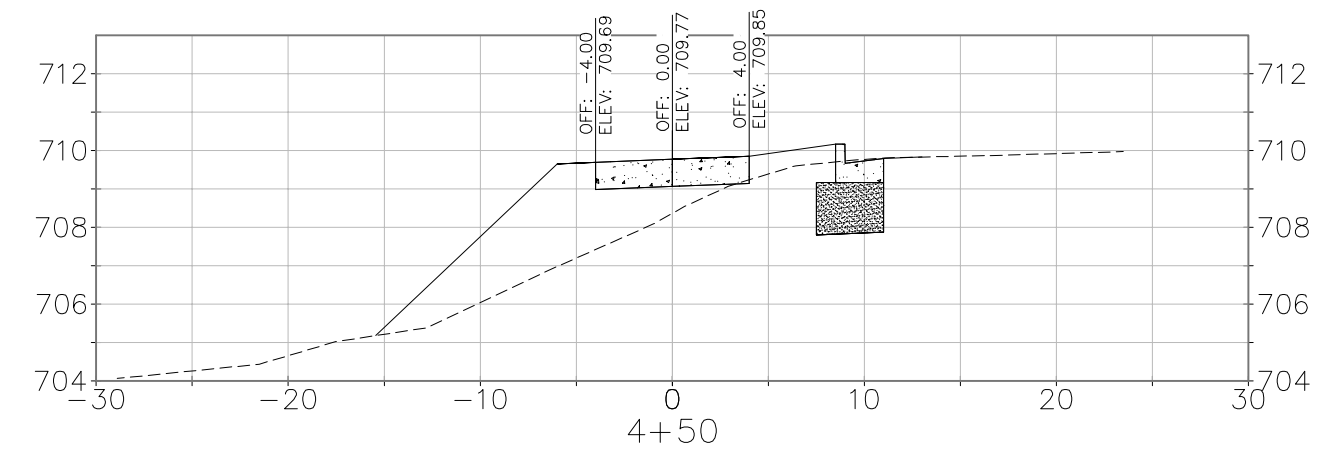
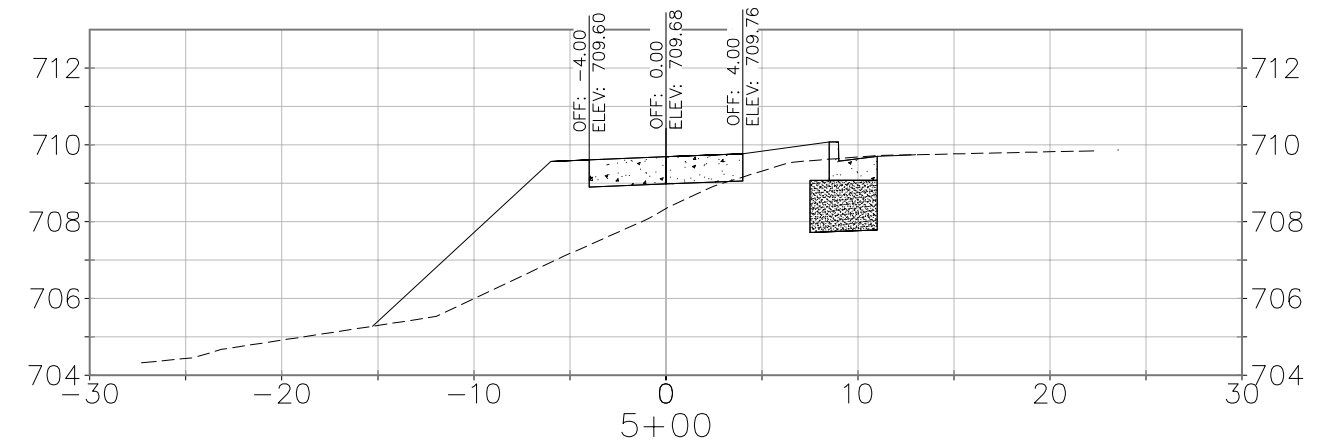


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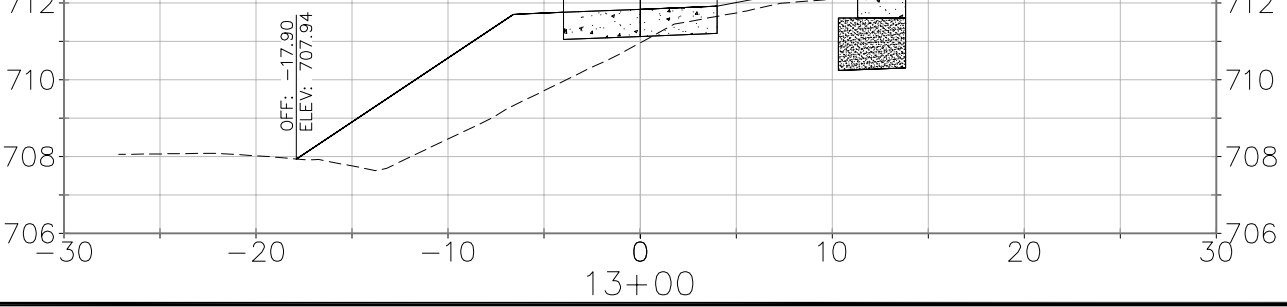
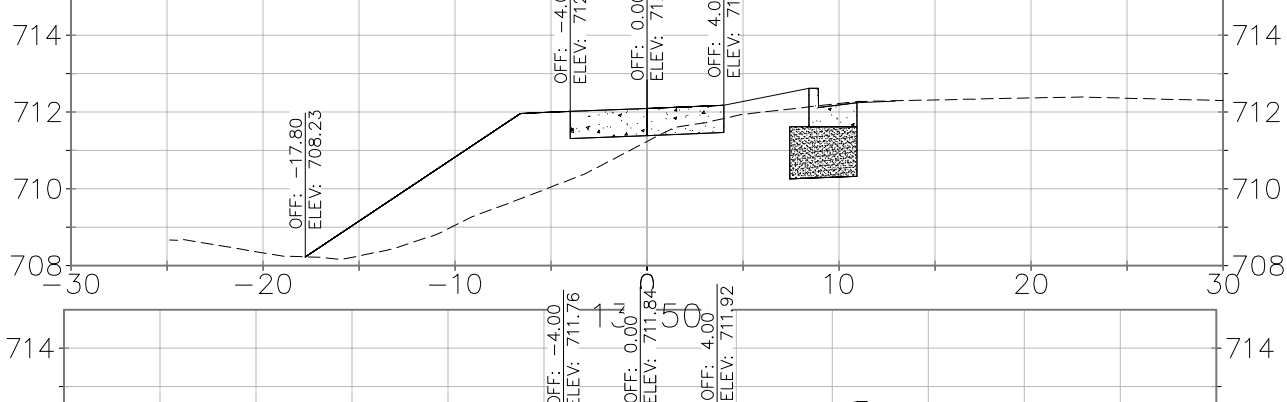
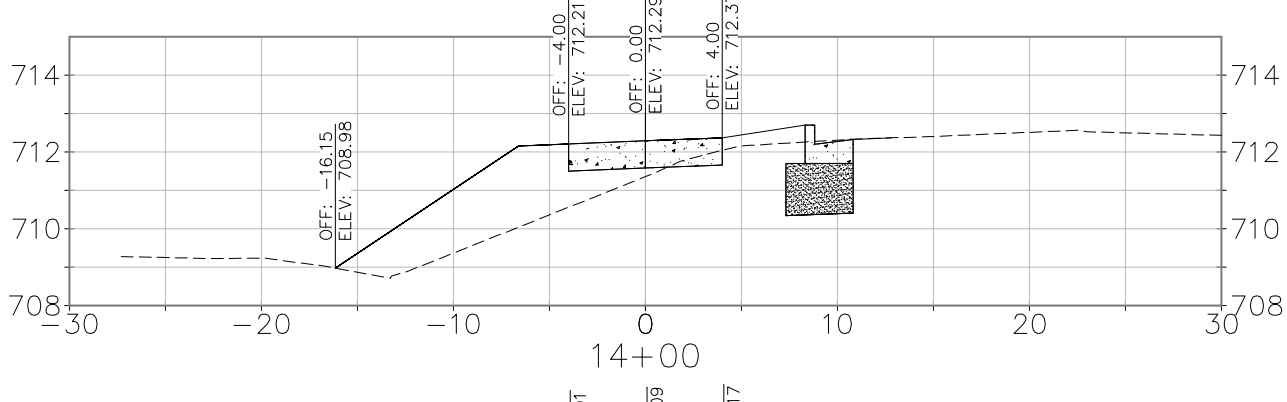
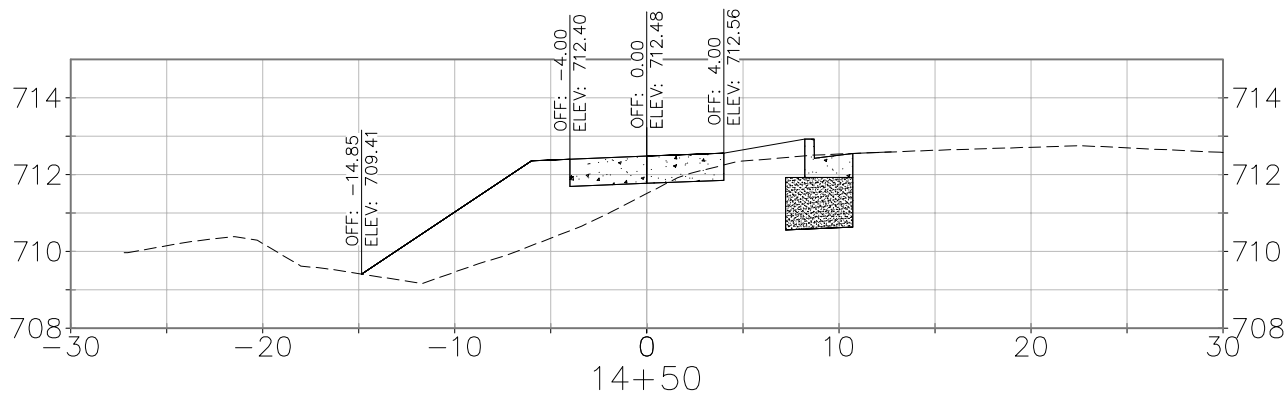
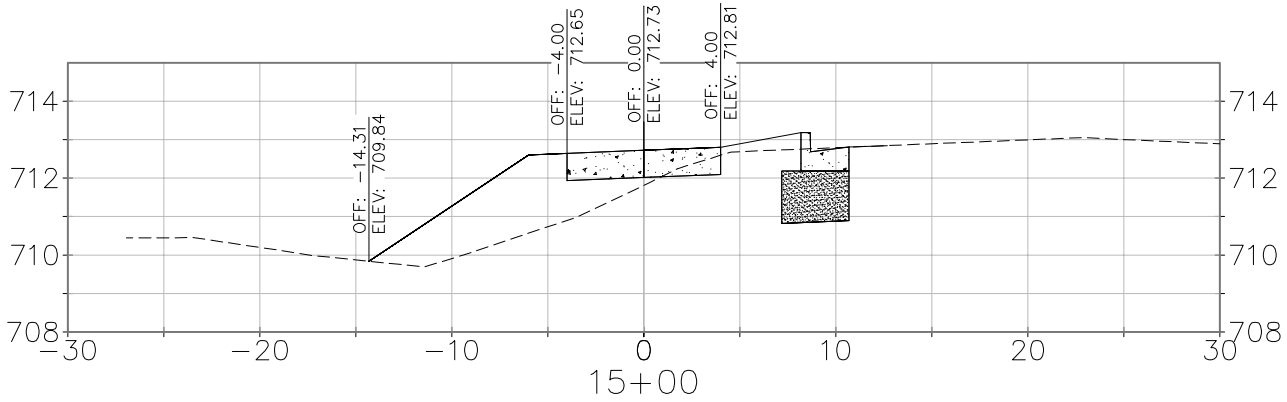
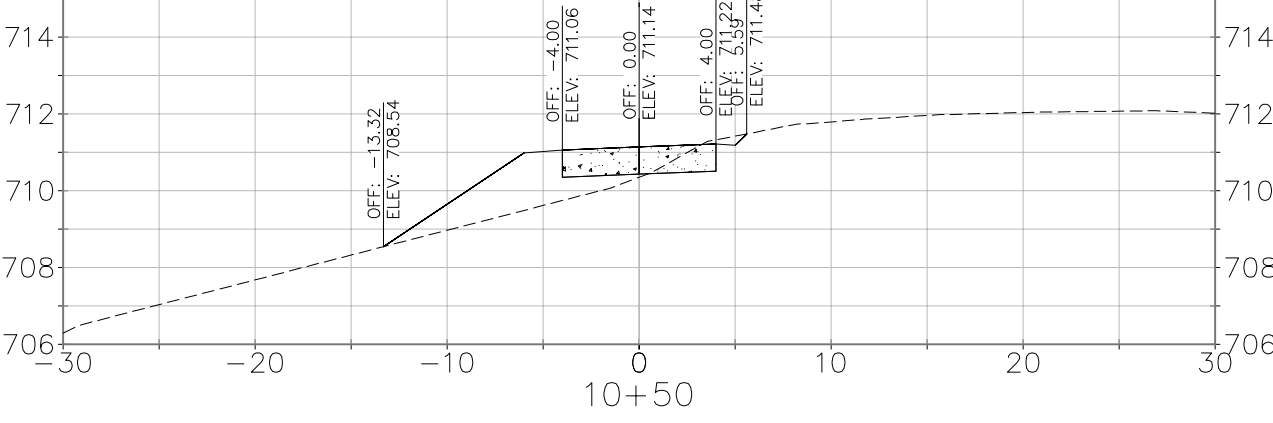
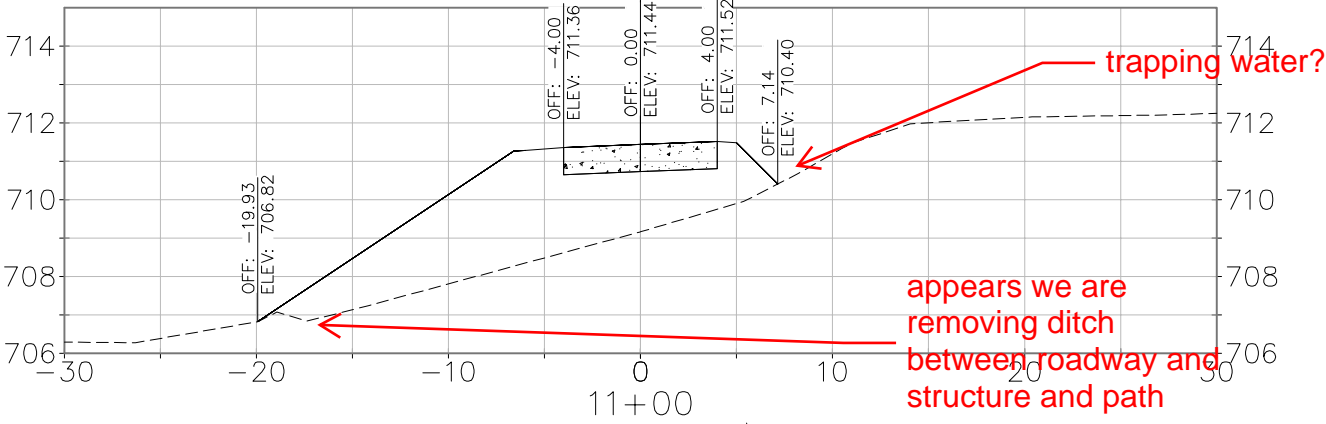
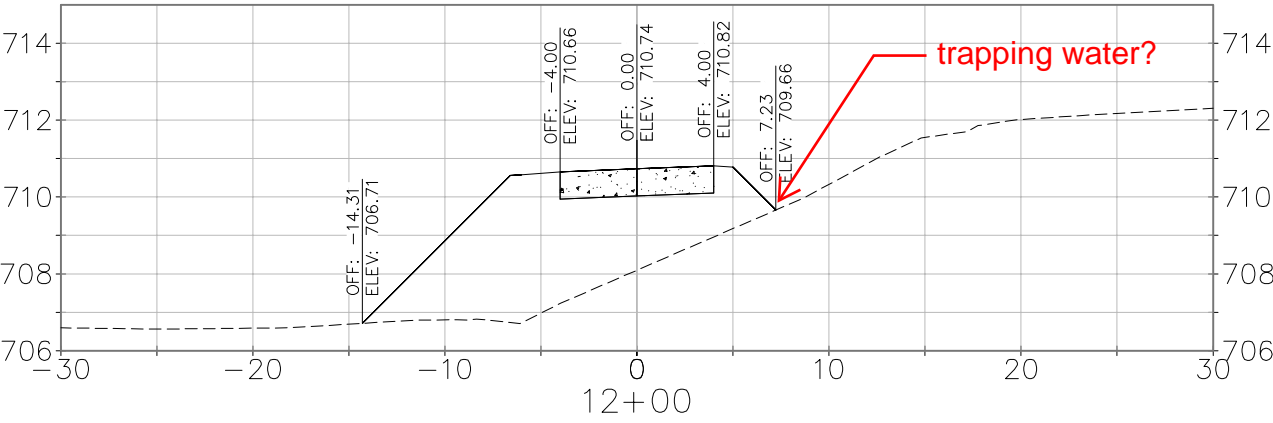
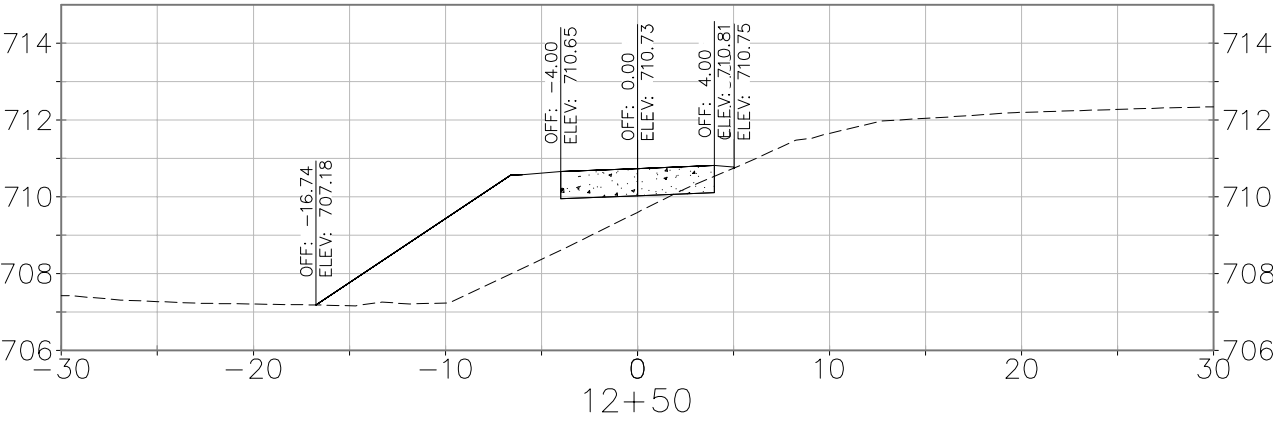


NEED CROSS SECTIONS TO INCLUDE:
slope intercept offset/elevations; right of
way lines and utilities



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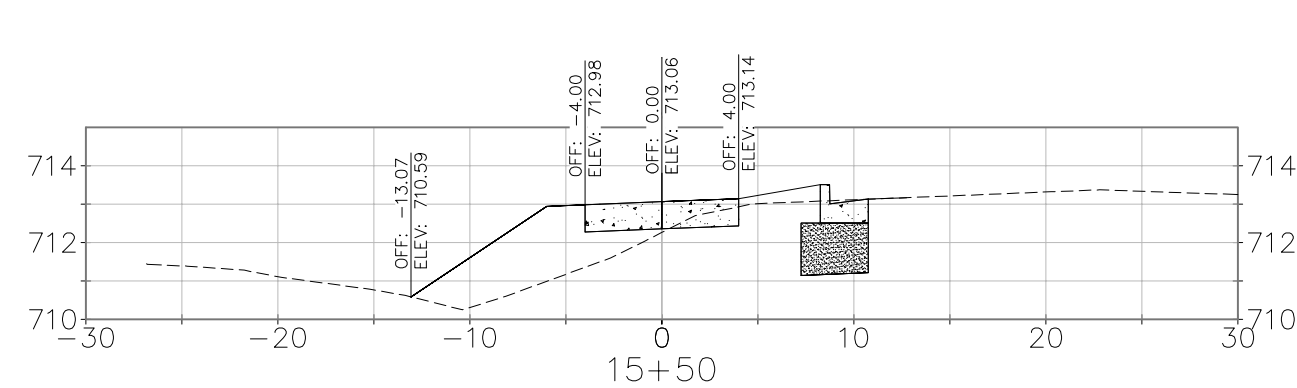
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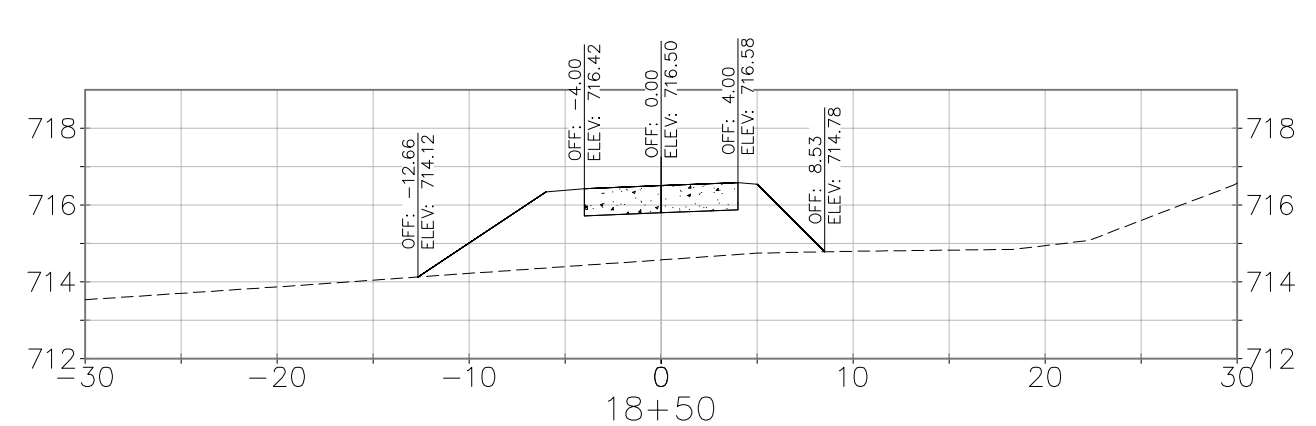
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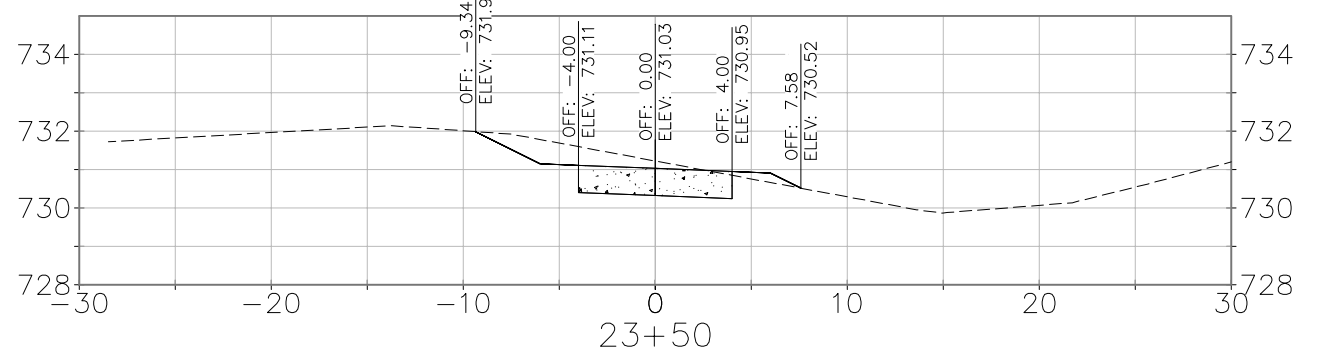
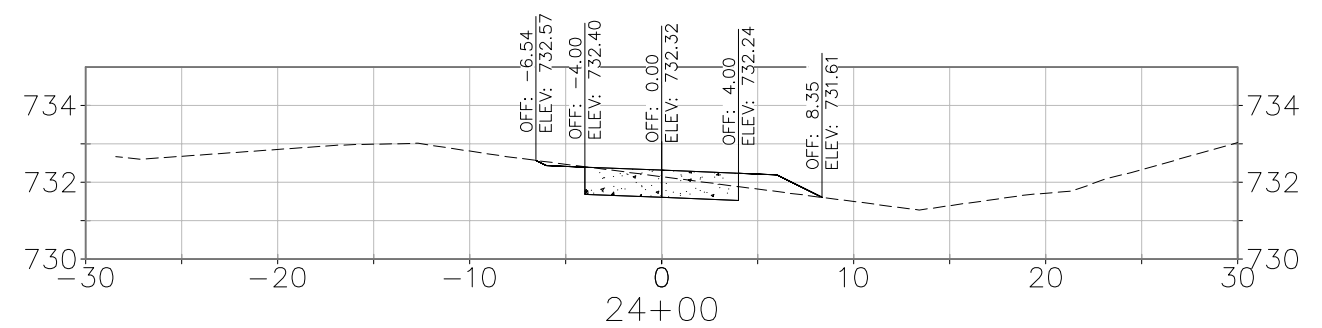
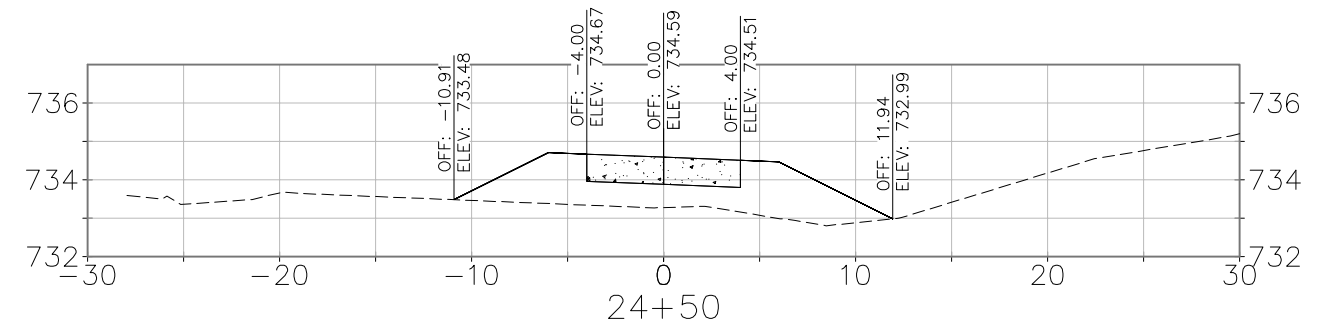
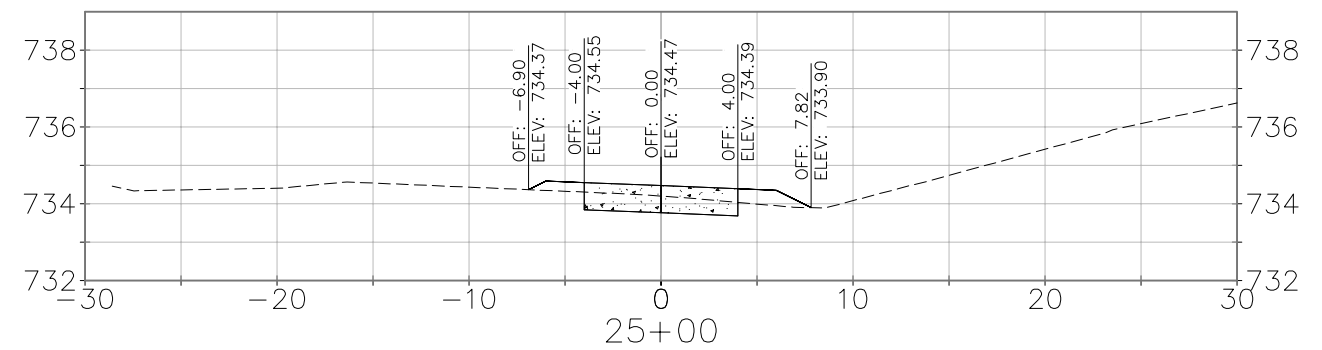
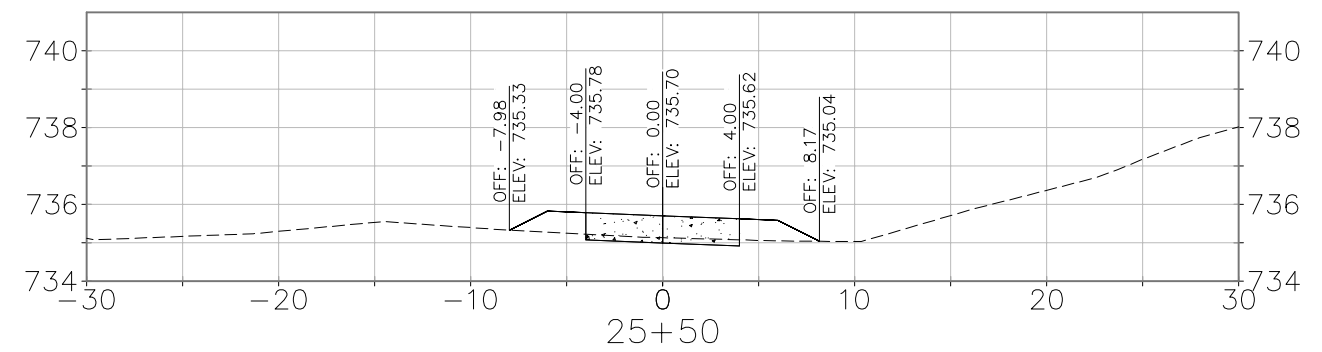
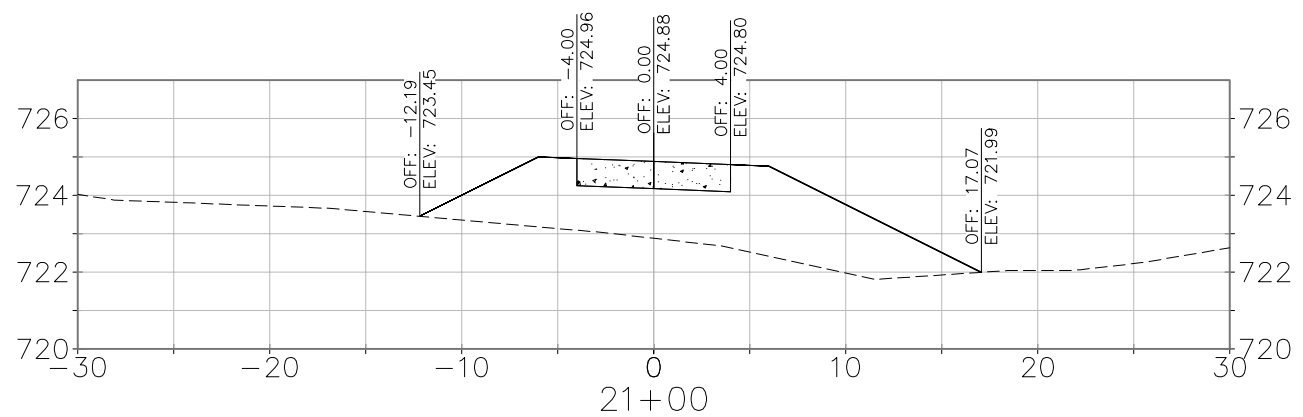
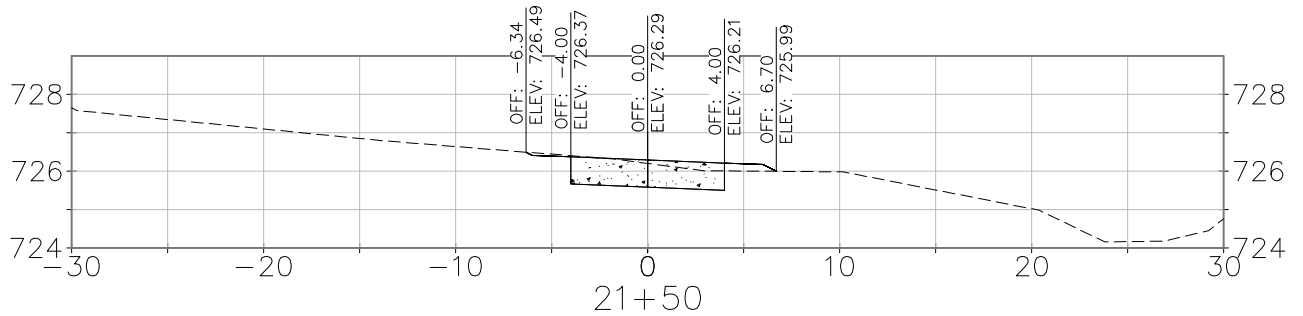
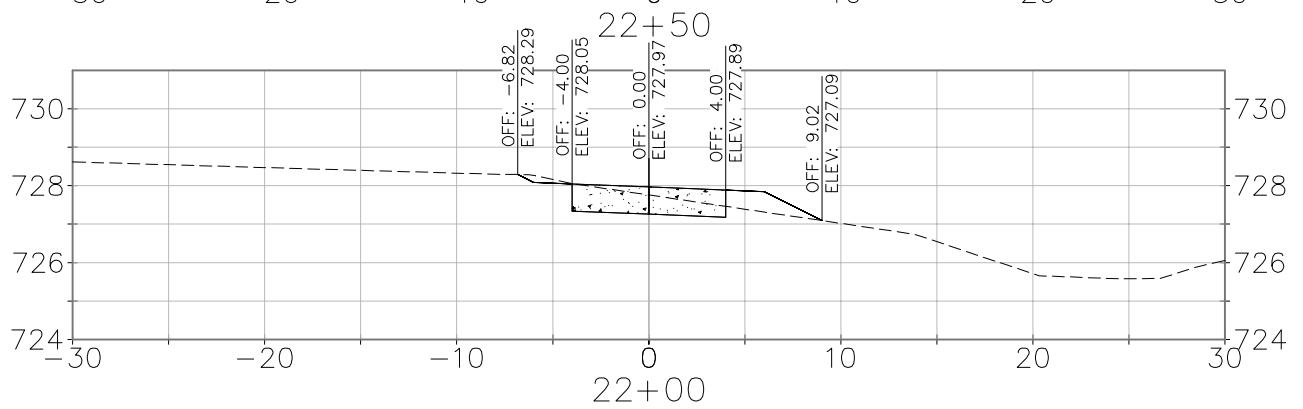
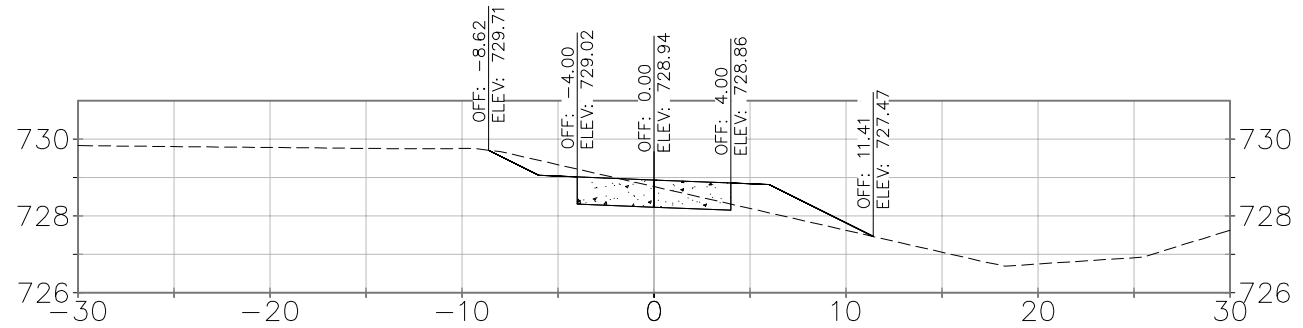
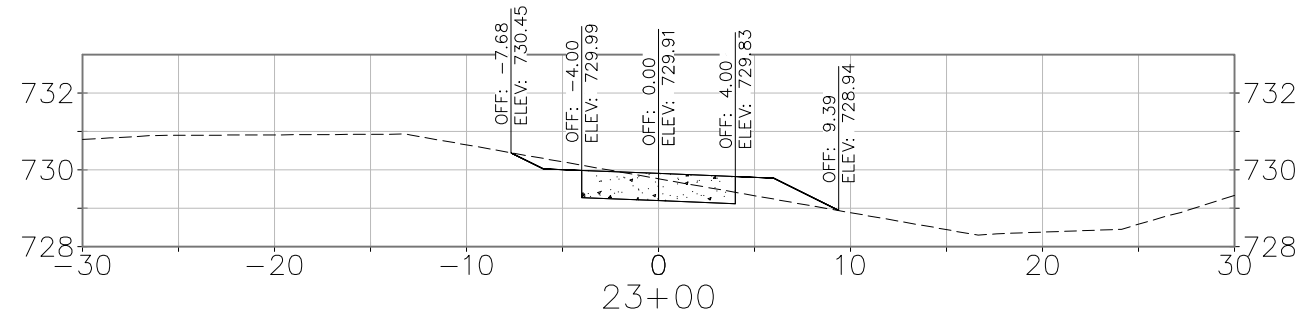


NEED CROSS SECTIONS TO INCLUDE:
right of way lines and utilities



SCALE AS SHOWN		REMARKS		REMARKS	
DATE	JULY 2018	NO.	DATE	NO.	DATE
AFTER					
BEFORE					
CHECKED	MMUC				
PROJECT NO.					
180089					
SHEET					
16 OF 24					
DWG. NO.					

NEED CROSS SECTIONS TO INCLUDE:
right of way lines and utilities



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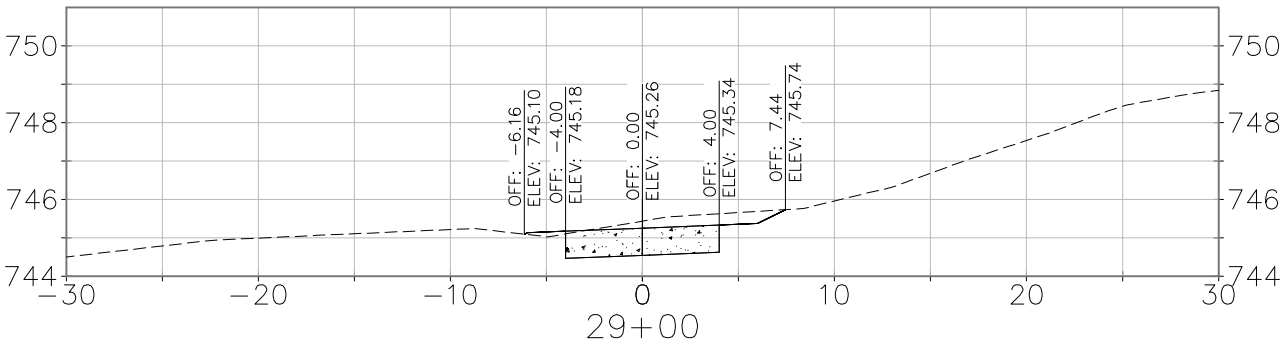
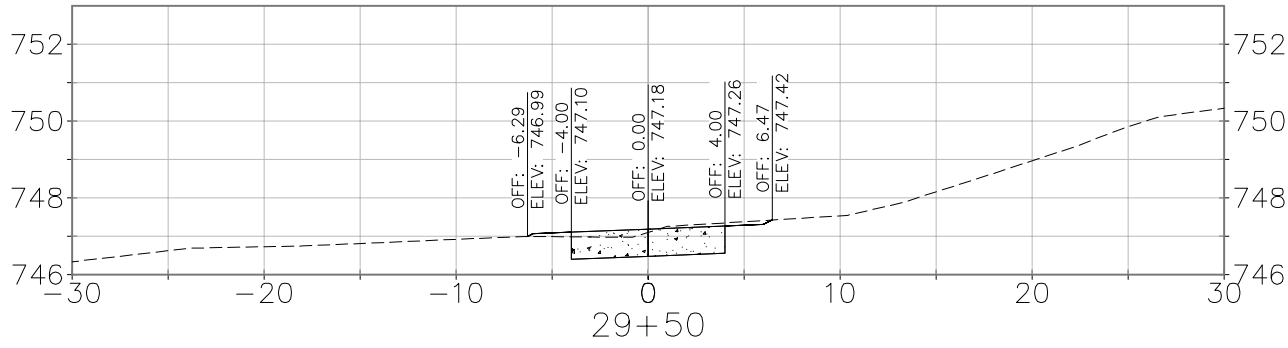
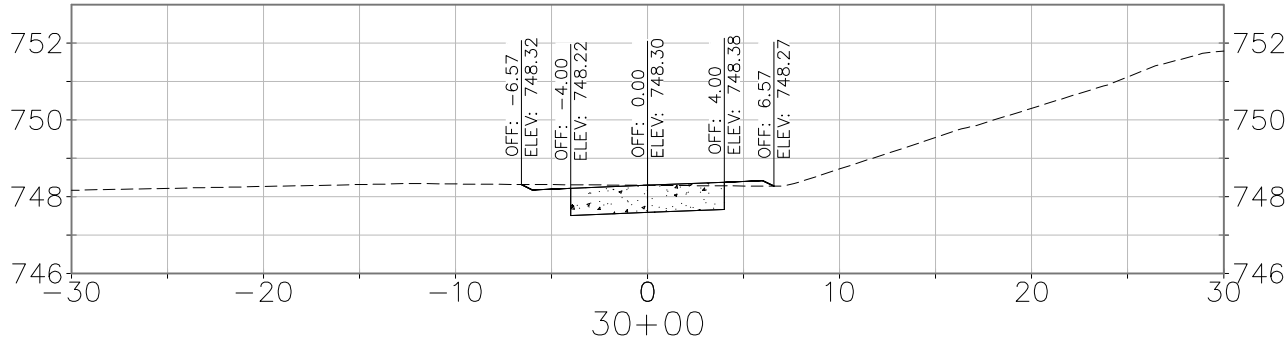
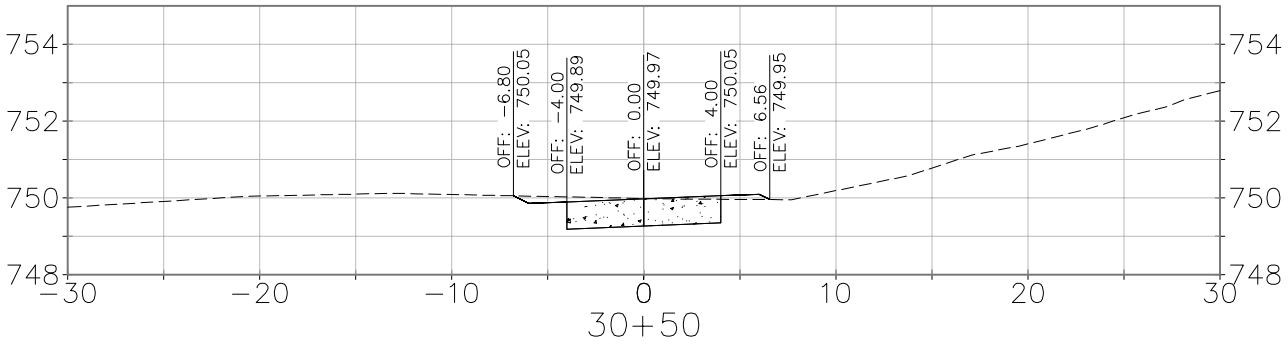
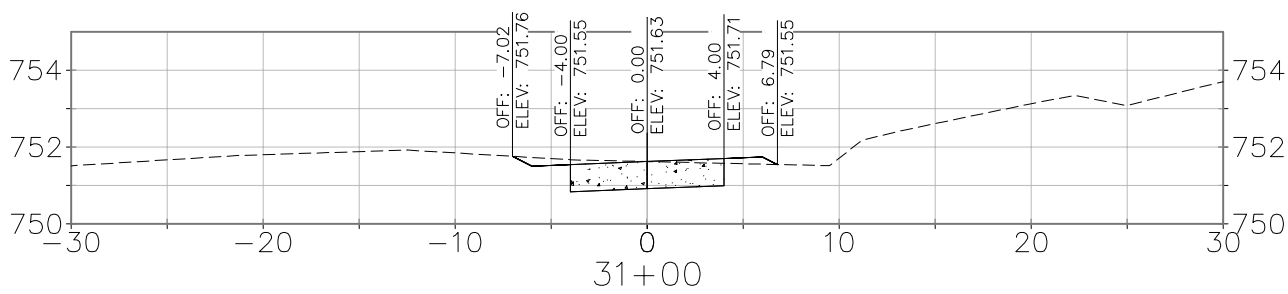
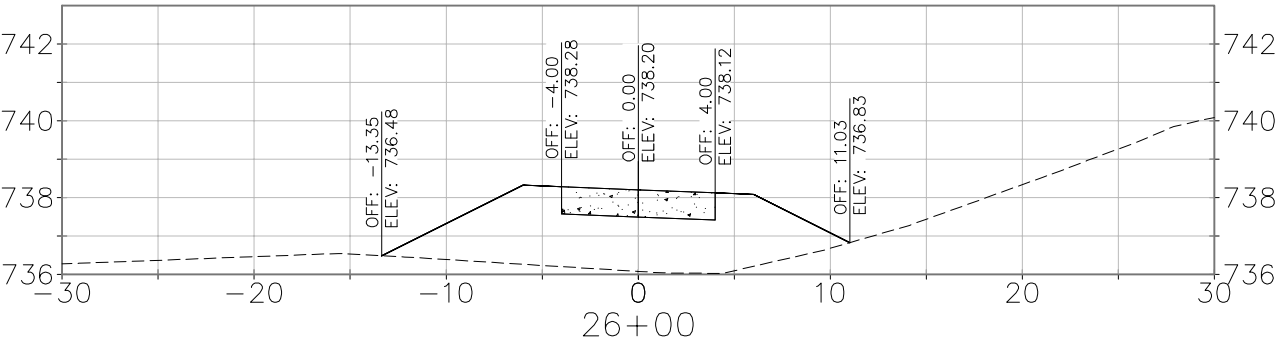
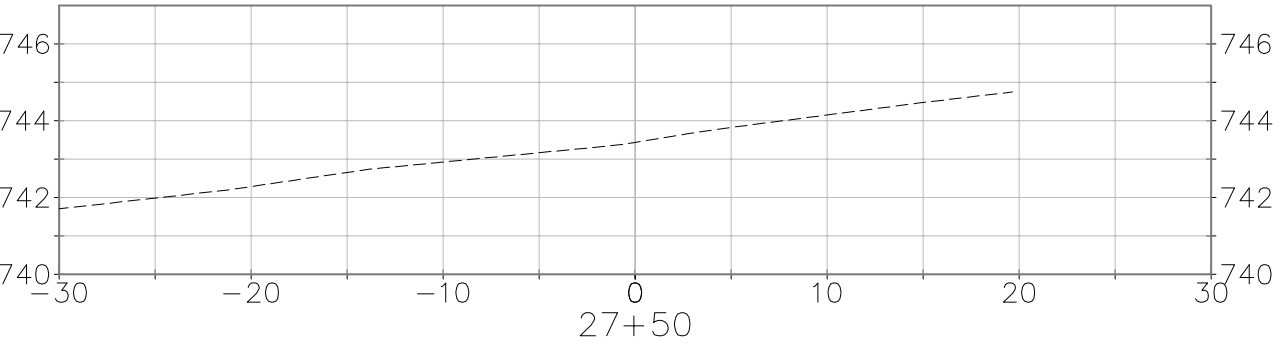
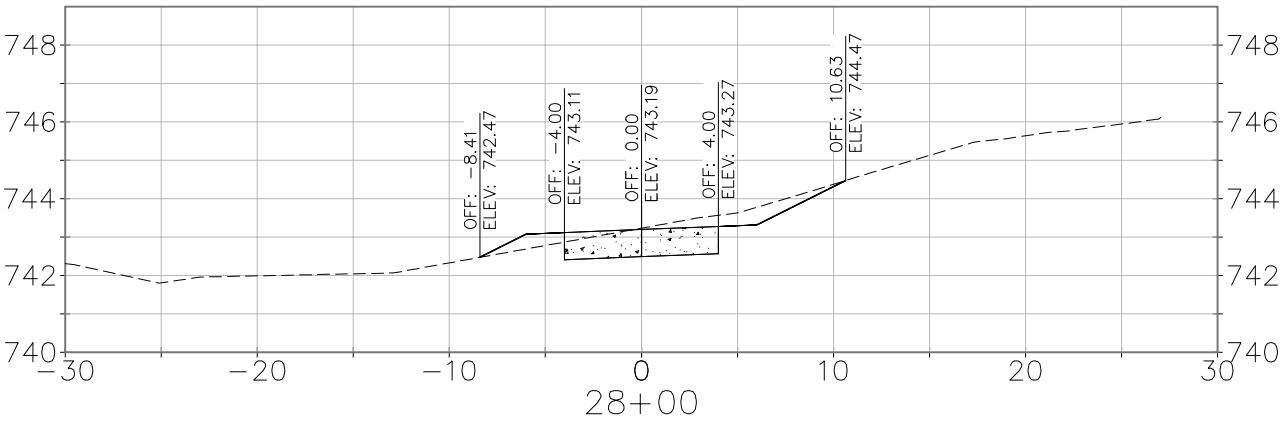
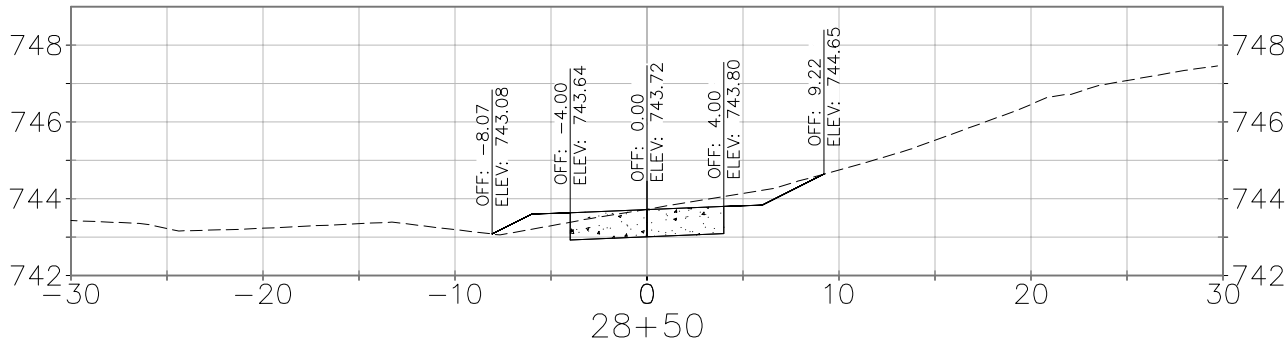
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CROSS SECTIONS

2018 TID NO. 1 INFRASTRUCTURE
TRAIL IMPROVEMENTS
GAYS MILLS, WISCONSIN

SCALE AS SHOWN		REVISIONS		REVISIONS	
DATE	JULY 2018	NO.	DATE	NO.	DATE
DRAFTER	PJUN				
CHECKED	MMUC				
PROJECT NO.	180089				
SHEET	17 OF 24				
DWG. NO.					

NEED CROSS SECTIONS TO INCLUDE:
right of way lines and utilities

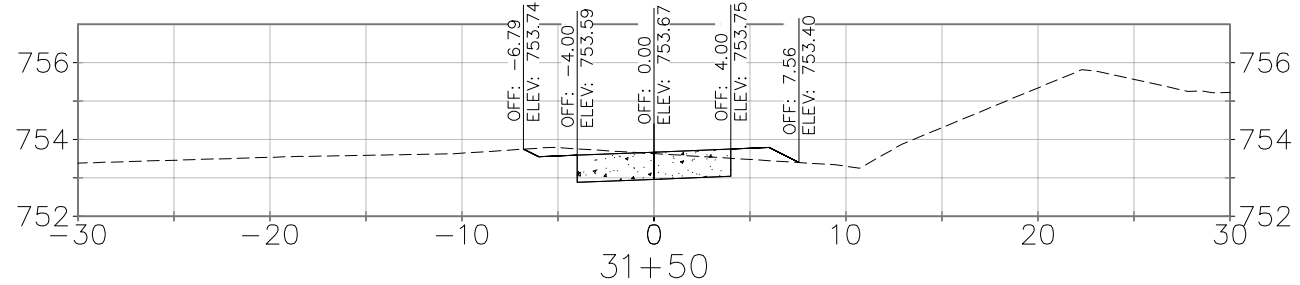
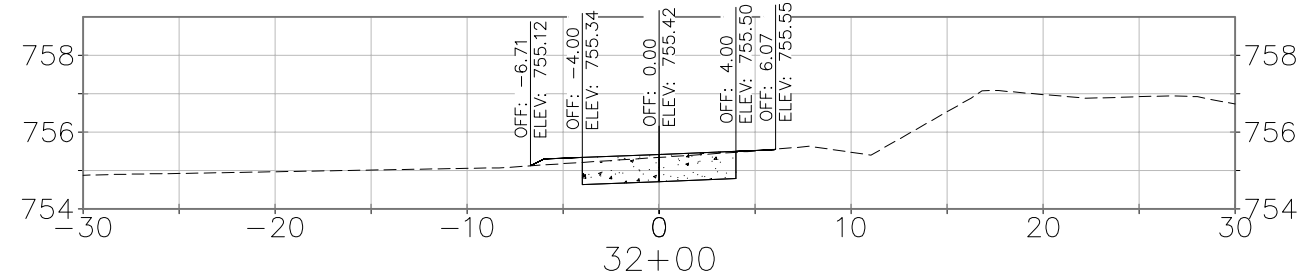
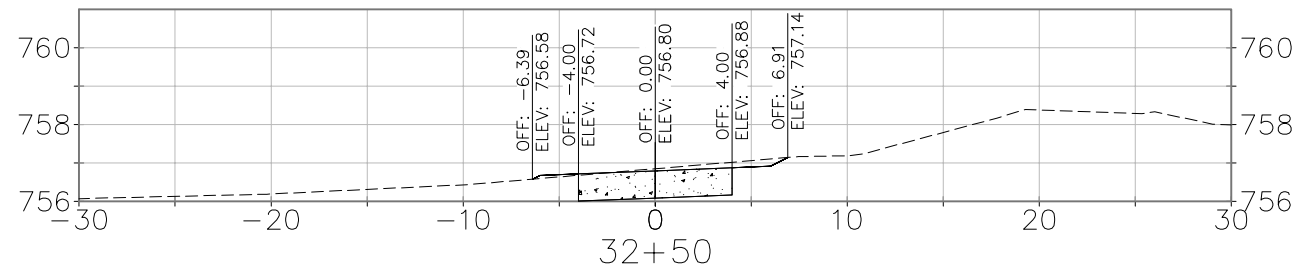
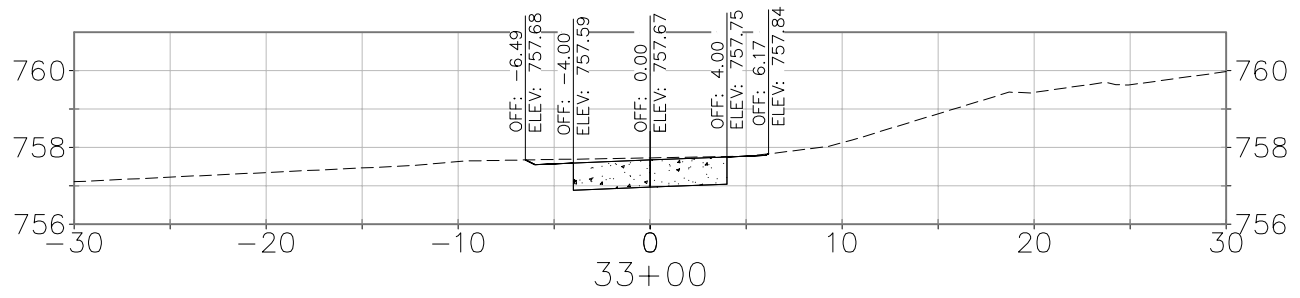
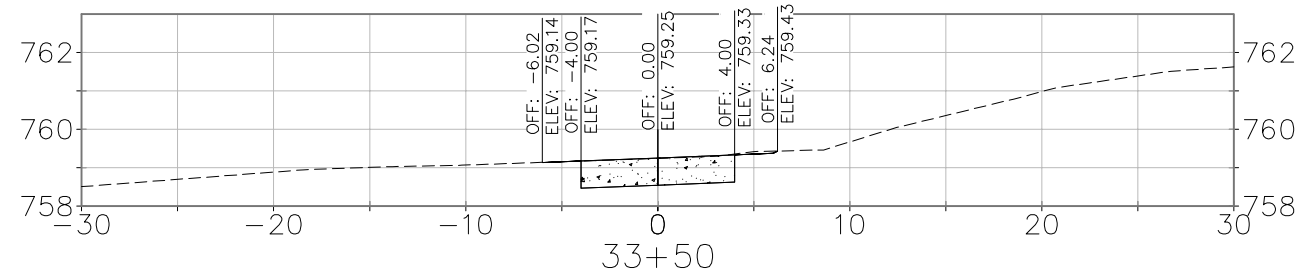


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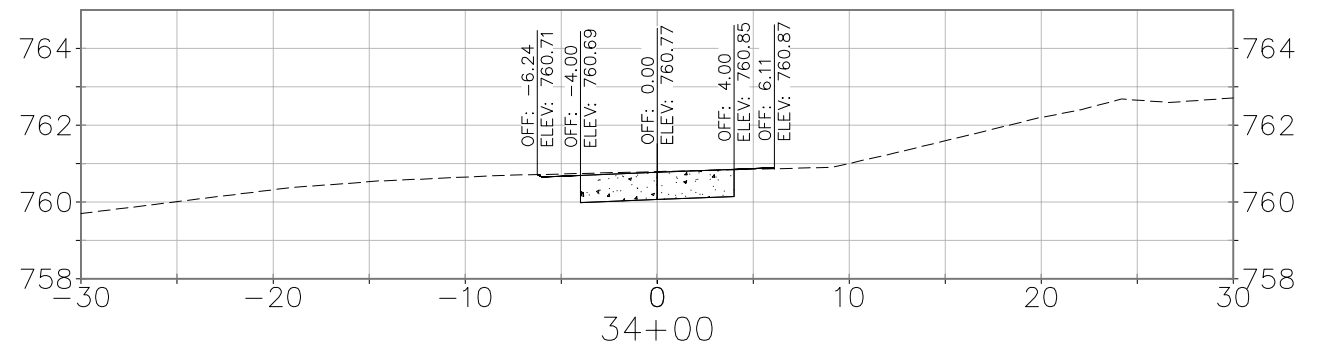
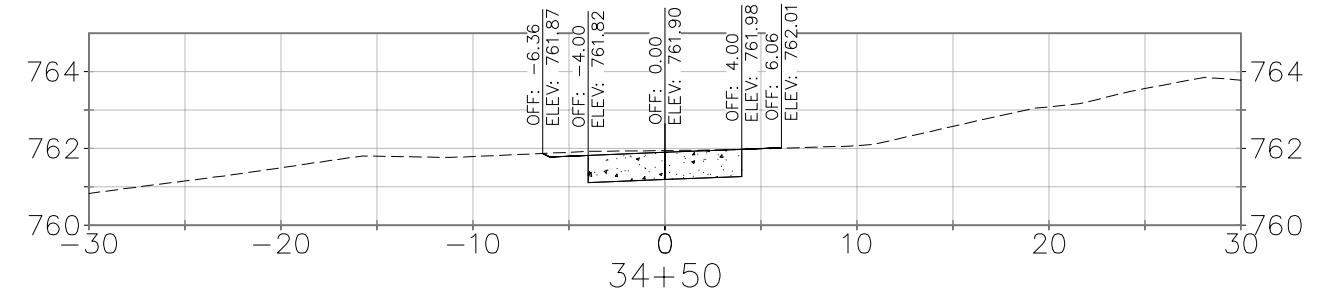
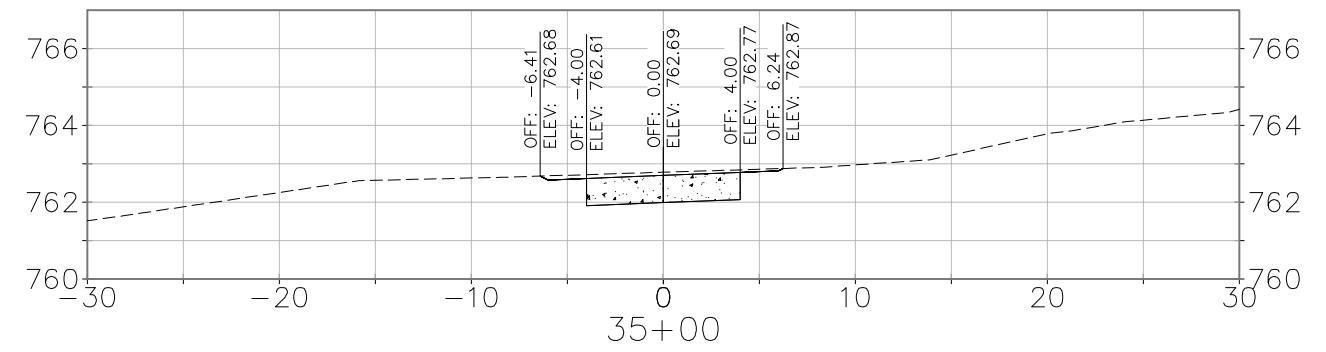
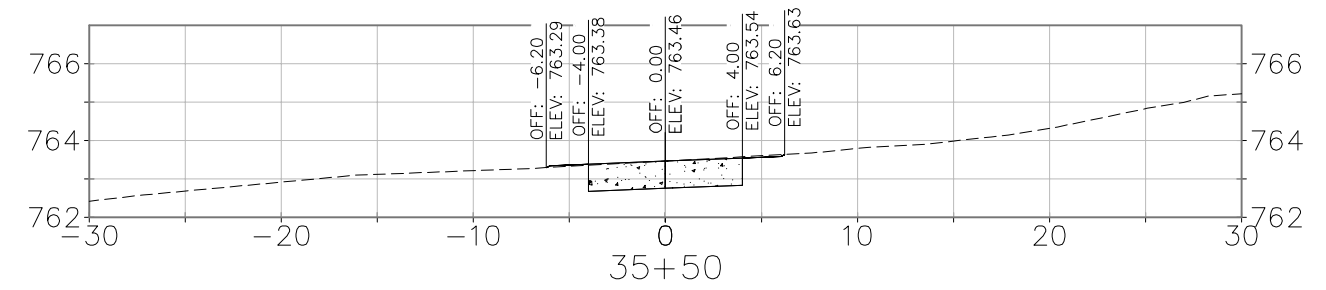
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CROSS SECTIONS
2018 TID NO. 1 INFRASTRUCTURE
TRAIL IMPROVEMENTS
GAYS MILLS, WISCONSIN

REV. NO.	DATE	REVISIONS	NO.	DATE	REVISIONS
SCALE AS SHOWN					
DATE JULY 2018					
DRAFTER PJUN					
CHECKED MMUC					
PROJECT NO. 180089					
SHEET 18 OF 24					
DWG. NO.					

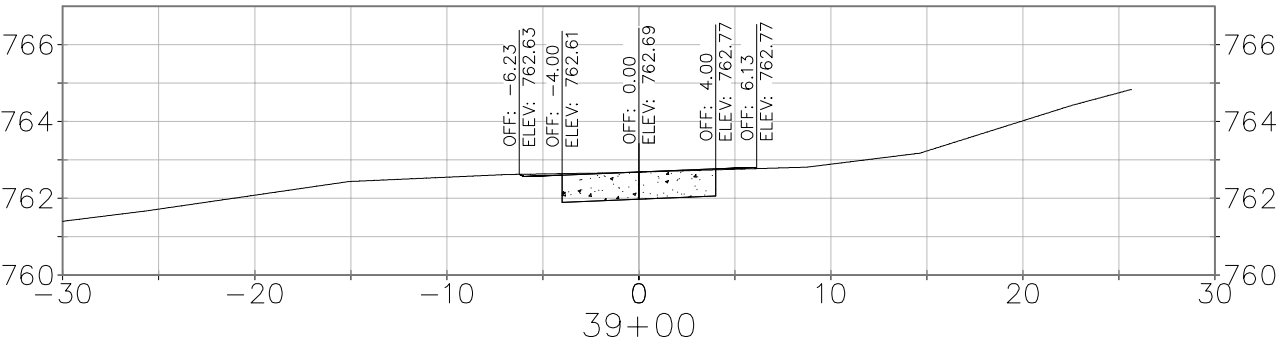
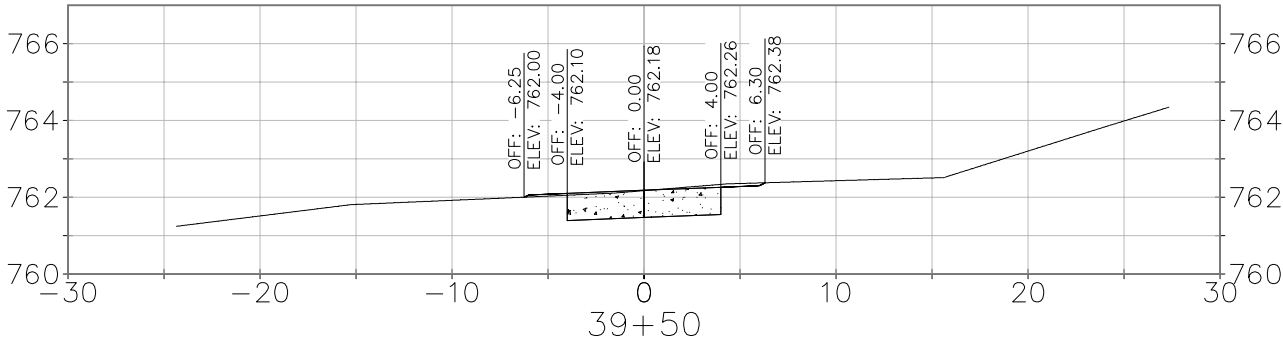
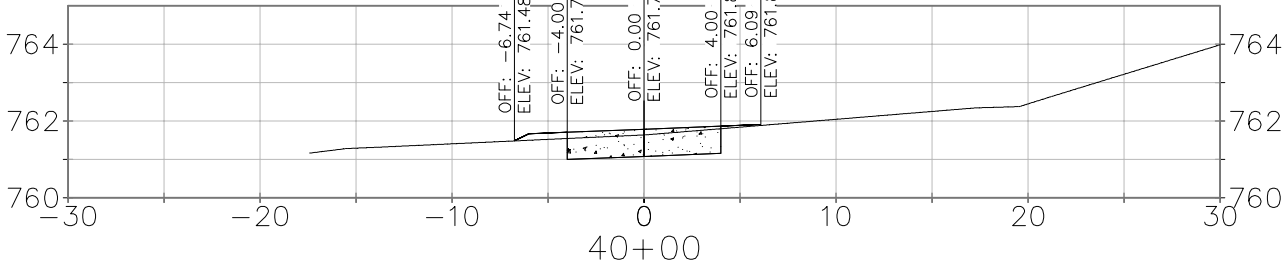
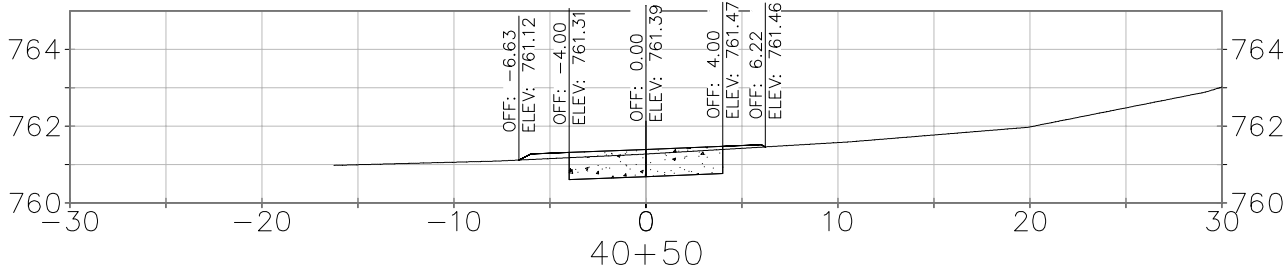
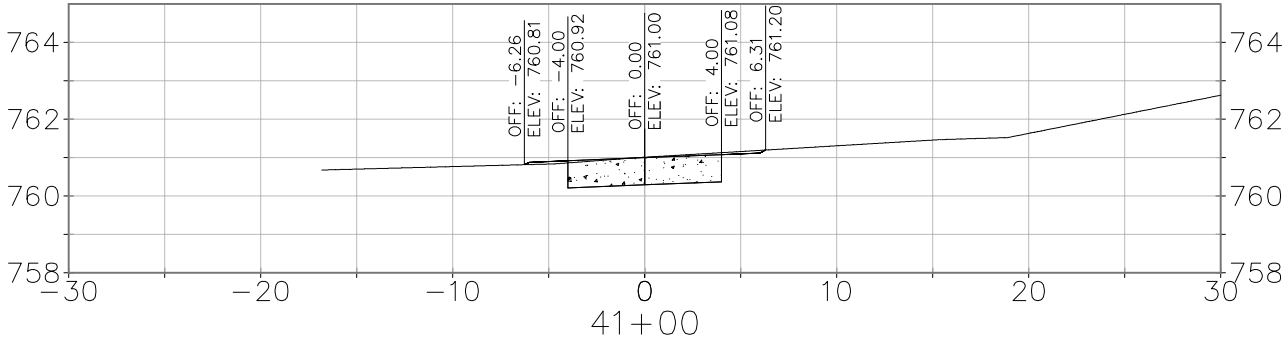
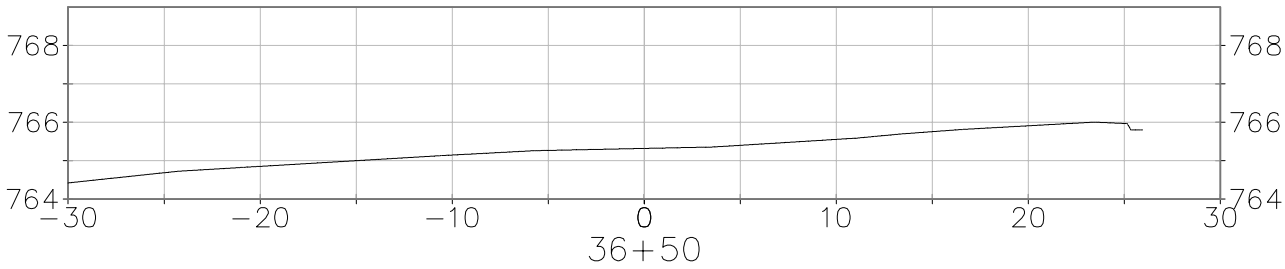
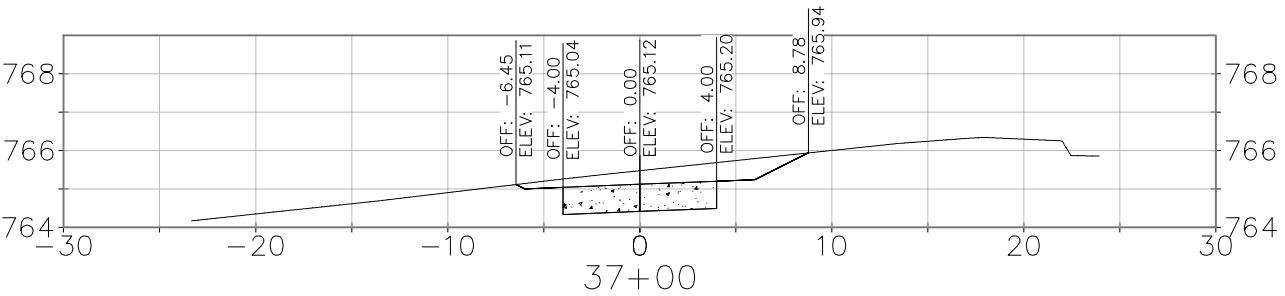
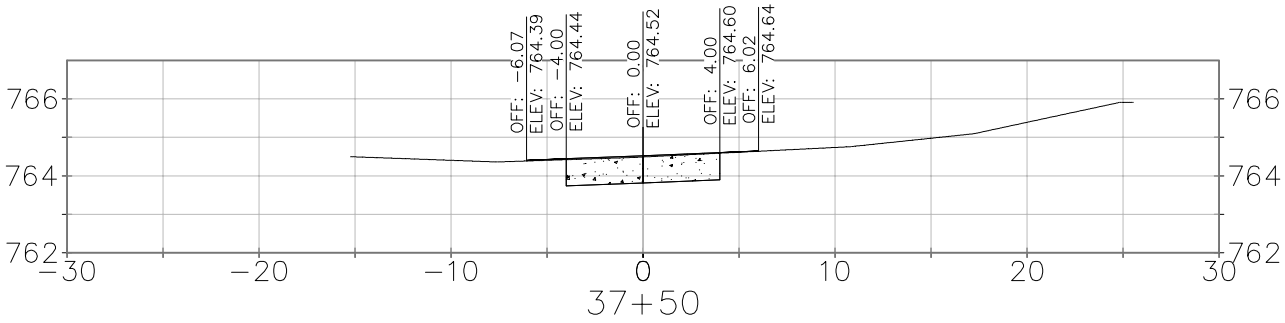
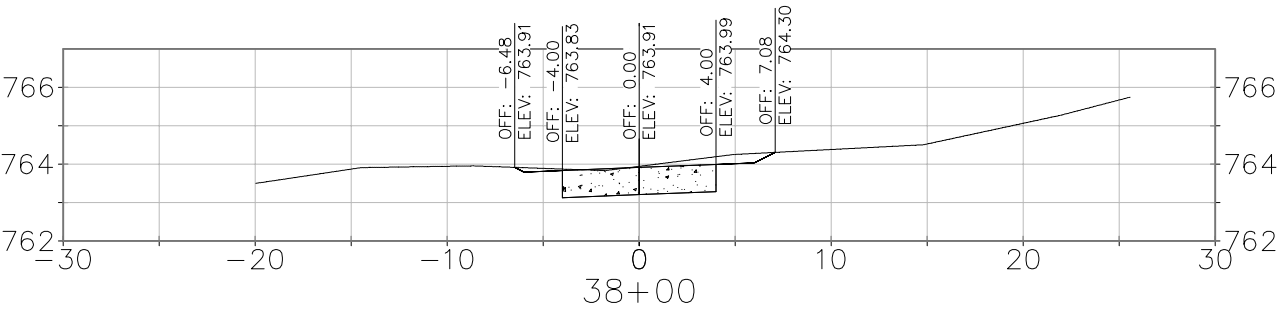
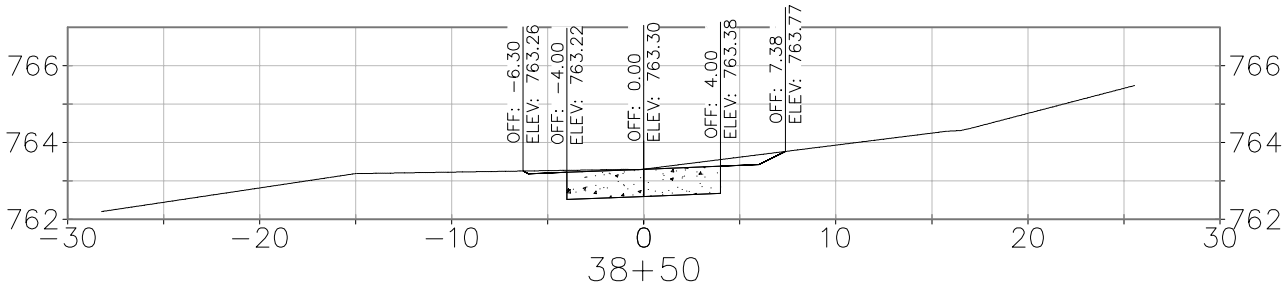


NEED CROSS SECTIONS TO INCLUDE:
right of way lines and utilities



DATE AS SHOWN		REMARKS		NO. DATE		REMARKS		NO. DATE		REMARKS	
JULY 2018											
DRAFTER											
CHECKED											
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PROJECT NO.											
180089											
SHEET											
19 OF 24											
DWG. NO.											

NEED CROSS SECTIONS TO INCLUDE:
right of way lines and utilities



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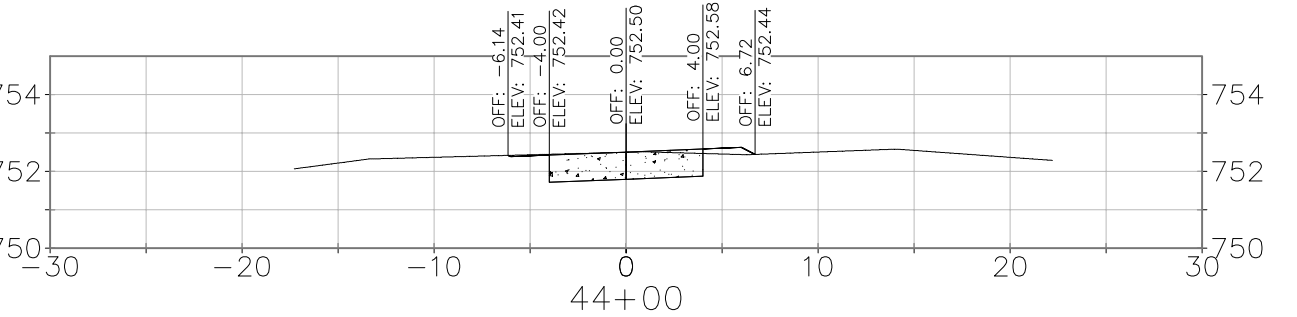
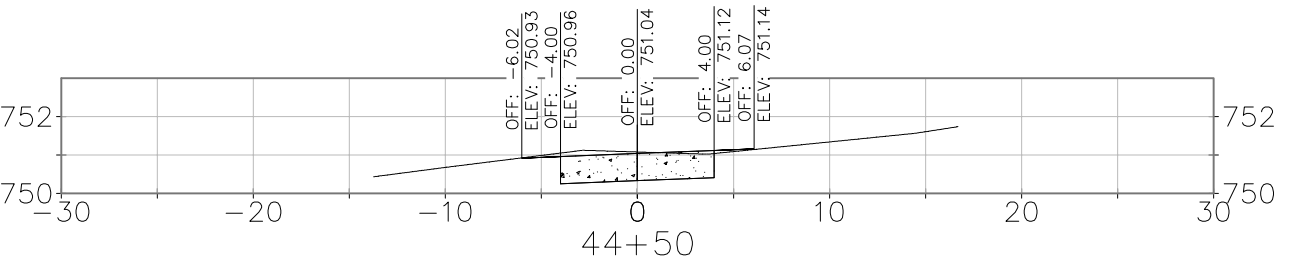
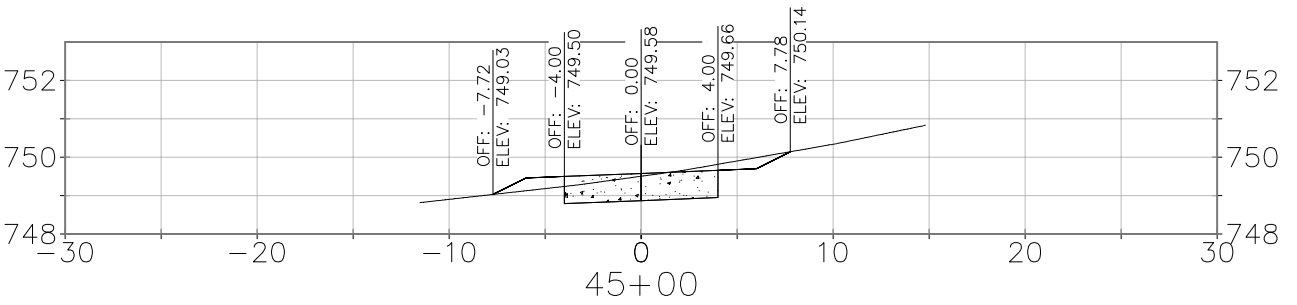
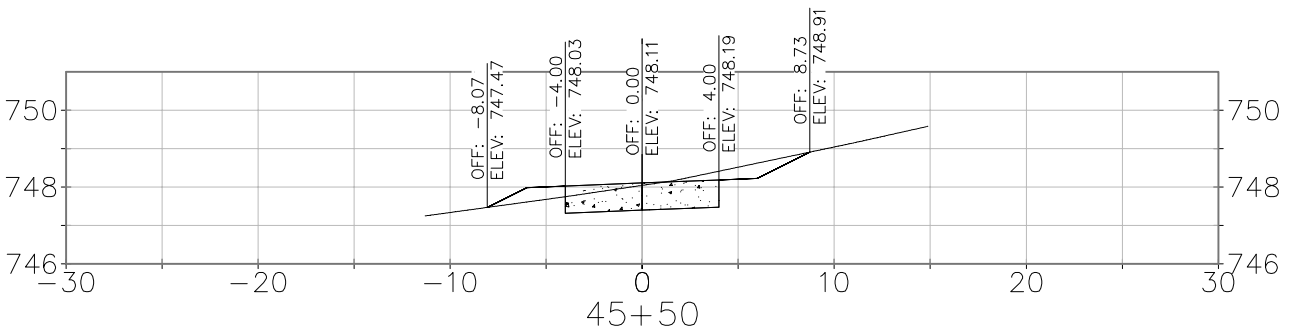
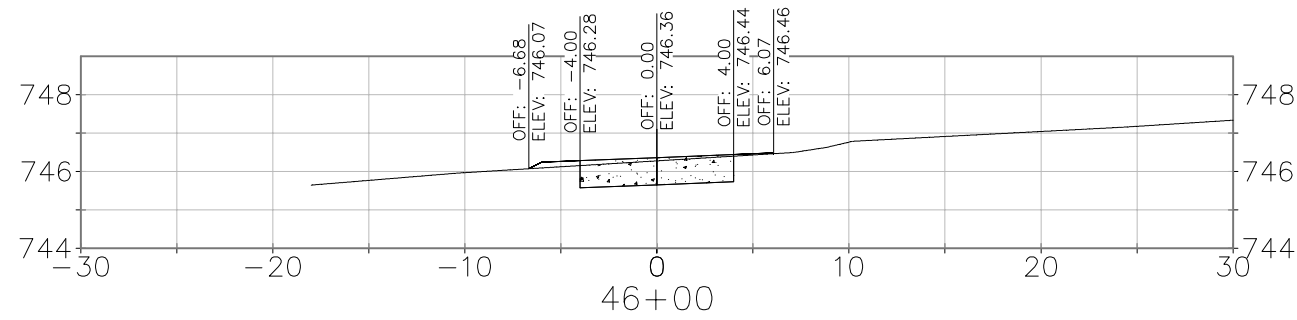
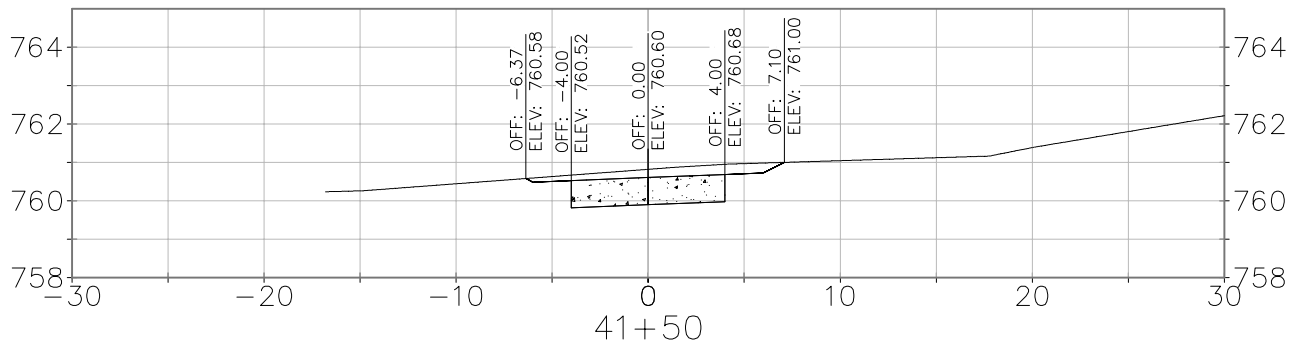
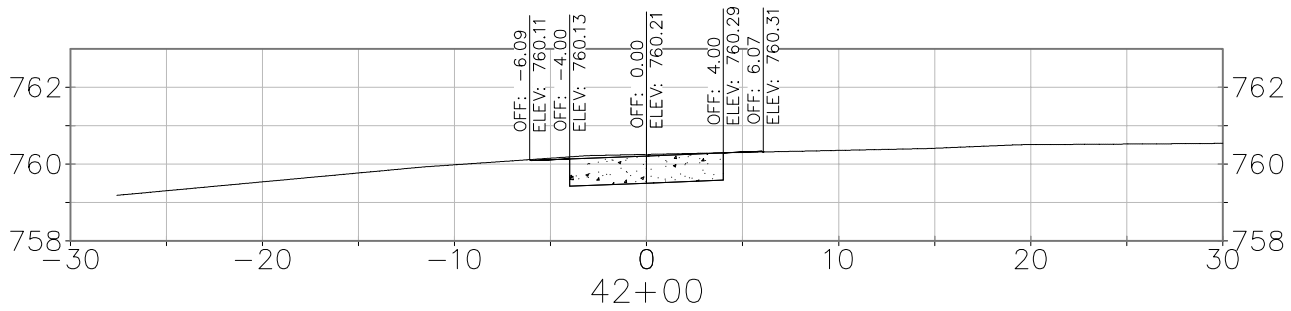
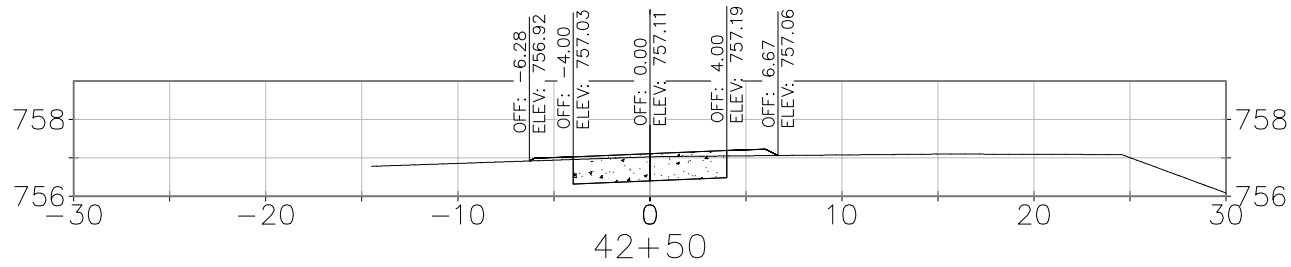
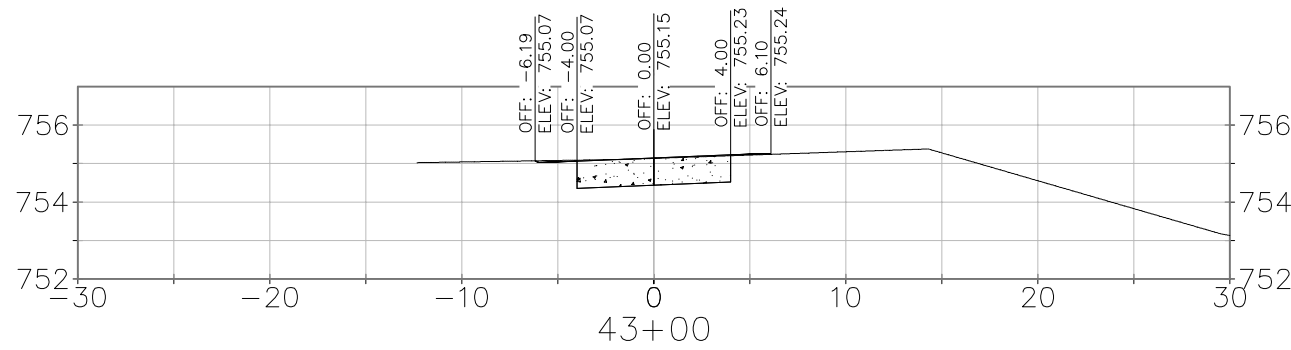
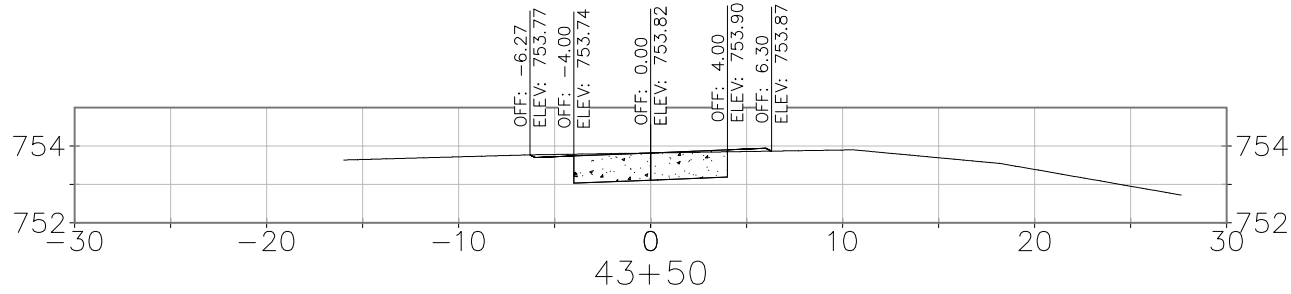
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CROSS SECTIONS
2018 TID NO. 1 INFRASTRUCTURE
TRAIL IMPROVEMENTS
GAYS MILLS, WISCONSIN

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

SCALE AS SHOWN
DATE JULY 2018
DRAFTER P.JUN
CHECKED MMUC
PROJECT NO. 180089
SHEET 20 OF 24
DWG. NO.

NEED CROSS SECTIONS TO INCLUDE:
right of way lines and utilities



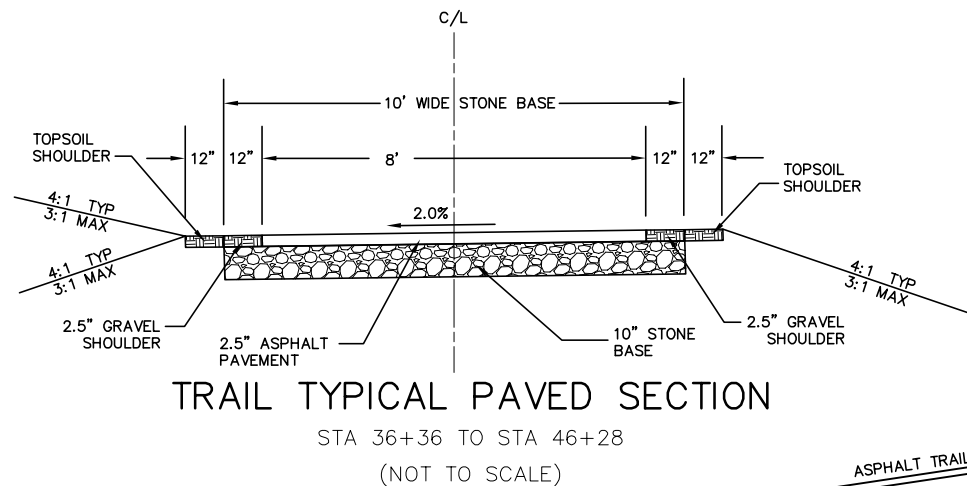
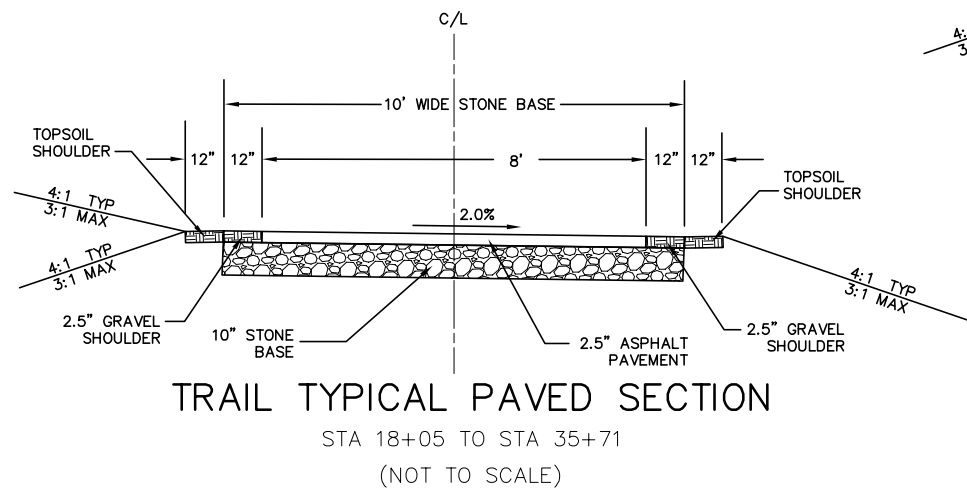
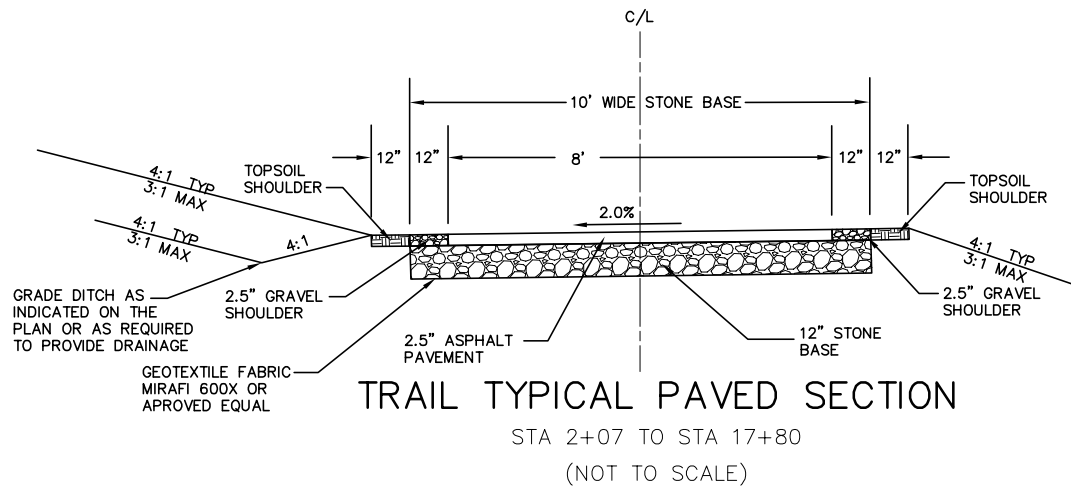
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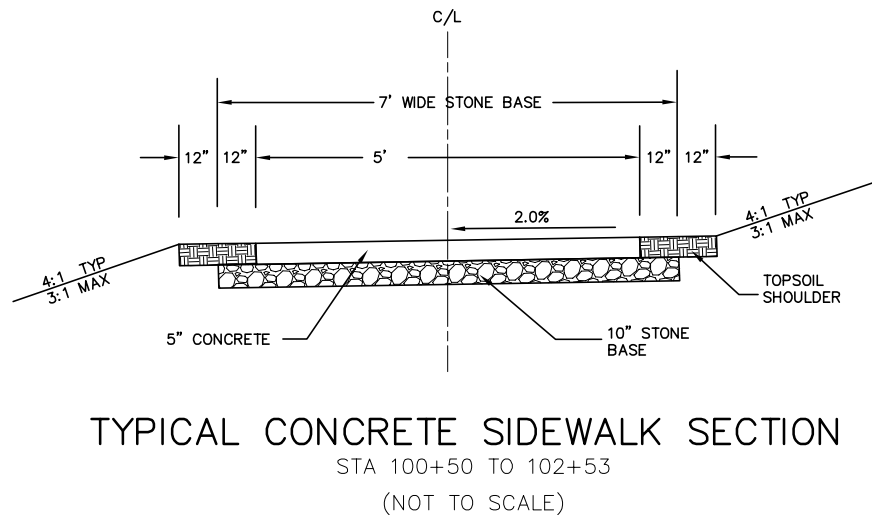
CROSS SECTIONS
2018 TID NO. 1 INFRASTRUCTURE
TRAIL IMPROVEMENTS
GAYS MILLS, WISCONSIN

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

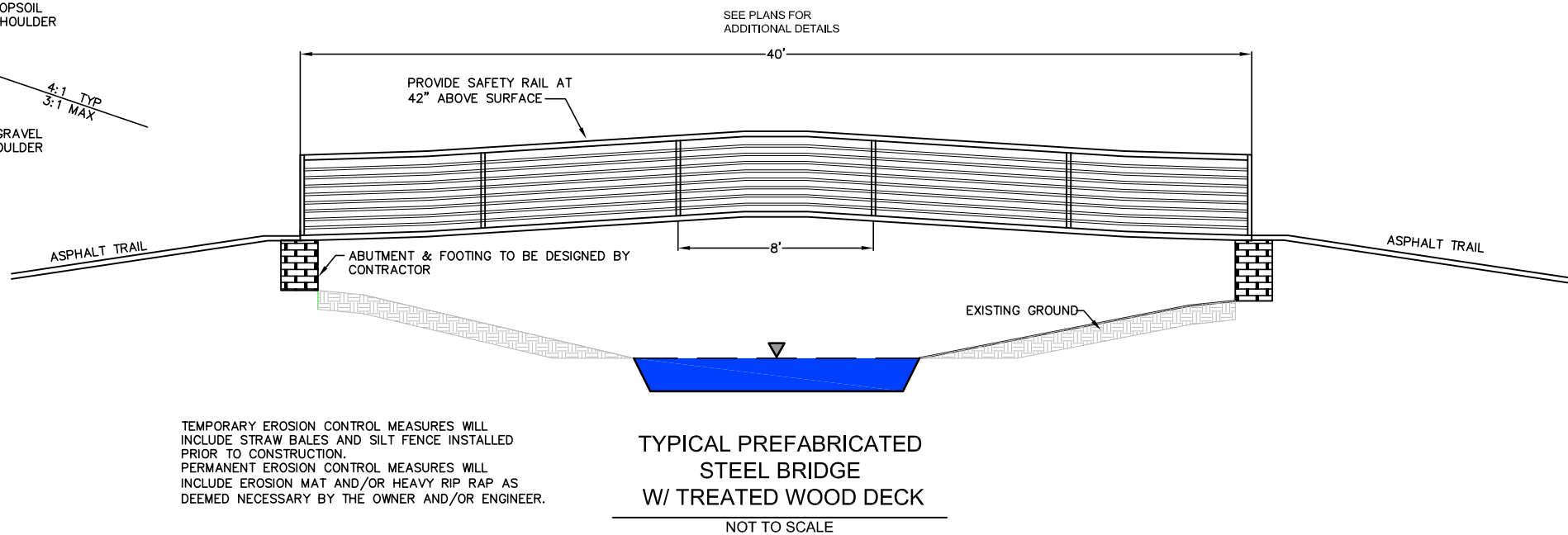
SCALE AS SHOWN
DATE JULY 2018
DRAFTER P.JUN
CHECKED MMUC
PROJECT NO. 180089
SHEET 21 OF 24
DWG. NO.



Need fence detail - fence should be added to typical sections as the clear width needs to be established - connection of fence to bridge rail?



Need to add typical sections that include WIS 131 or show relation to Clear Zone of WIS 131 in typical sections (ditch section traversability)



REVISIONS	NO.	DATE	REMARKS
SCALE AS SHOWN			
DATE			
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DRAFTER			
PJUN			
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SHEET			
22 OF 24			
DWG. NO.			

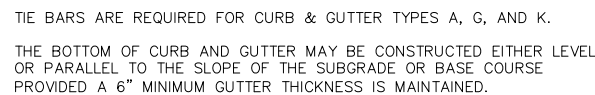


Diagram illustrating the placement of a stop sign and a trail end sign at a street intersection:

- STOP Sign:** R1-1, 18" x 18".
- Trail End Sign:** CUSTOM 1, 12" x 12", BLACK COPY ON WHITE.
- Sign Placement:** The trail end sign is placed above the stop sign, with a line indicating they are to be viewed together.
- Street Layout:** The intersection is labeled "STREET (OR TRAIL END)". The curb or edge of pavement is indicated.
- Trail Direction:** The trail is labeled "PAVED TRAIL" with an arrow pointing left. A red arrow labeled "AS NEEDED" points right.
- Distance:** A dimension line indicates a distance of 150.0' from the stop sign to the start of the trail.
- Additional Sign:** A "NO MOTOR VEHICLES" sign (R5-3, 18" x 18") is shown on the right side of the street, with a red arrow pointing to it.

TYPICAL TRAIL SIGN LOCATIONS

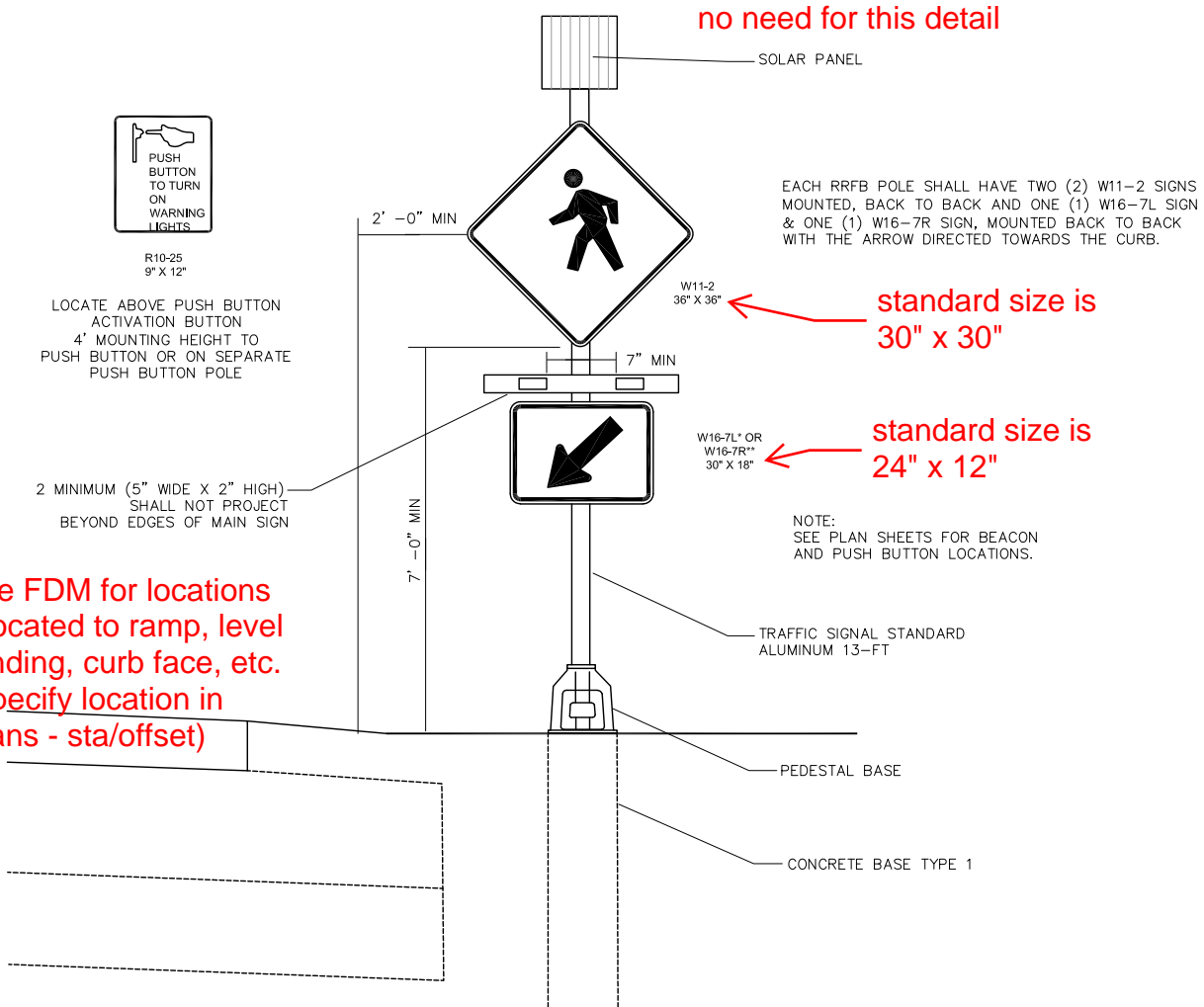
(NOT TO SCALE)



1 MOUNTABLE 36" CURB SECTION (TYPES A & D)
1 NOT TO SCALE



1 24" CONCRETE CURB AND GUTTER
1 NOT TO SCALE



1 1 RECTANGULAR RAPID FLASHING BEACON DETAIL NOT TO SCALE

BOLLARD NOTES:

1. MODEL: PB-B ROUND AS MANUFACTURED BY DURA ART STONE FONTANA,CA
2. CONSTRUCTION: GLASCRETE
3. COLOR: CHARCOAL GRAY
4. FINISH: LSB - LIGHT SANDBLAST
5. MOUNTING "C": REMOVEABLE AS SHOWN SET IN CONCRETE

TYPICAL REMOVABLE BOLLARD
(NOT TO SCALE)

Bollards not shown in plans - we suggested removal - if removed - no need for this detail

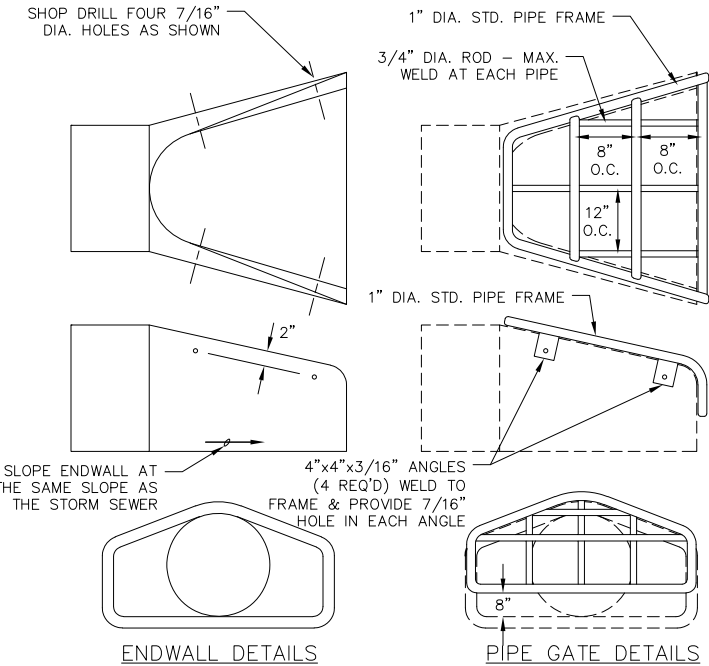
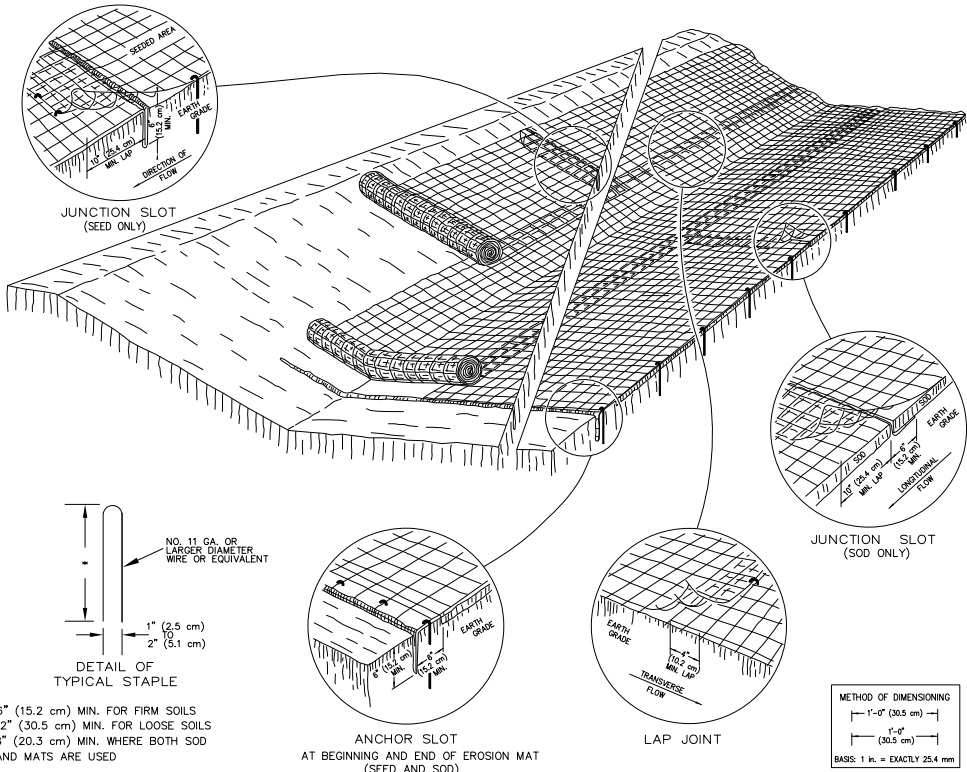
EACH RRFB POLE SHALL HAVE TWO (2) W11-2 SIGNS MOUNTED, BACK TO BACK AND ONE (1) W16-7L SIGN & ONE (1) W16-7R SIGN, MOUNTED BACK TO BACK WITH THE ARROW DIRECTED TOWARDS THE CURB.

- standard size is 30" x 30"

- standard size is 24" x 12"

NOTE:
SEE PLAN SHEETS FOR BEACON
AND PUSH BUTTON LOCATIONS.

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DETAILS			
SCALE		2018 TID NO. 1 INFRASTRUCTURE TRAIL IMPROVEMENTS	
AS SHOWN		GAYS MILLS, WISCONSIN	
DATE	JULY 2018		
DRAWN BY	PJUN		
CHECKED BY	MMUC		
PROJECT NO.	180089		
SHEET	23 OF 24		
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NOTES:

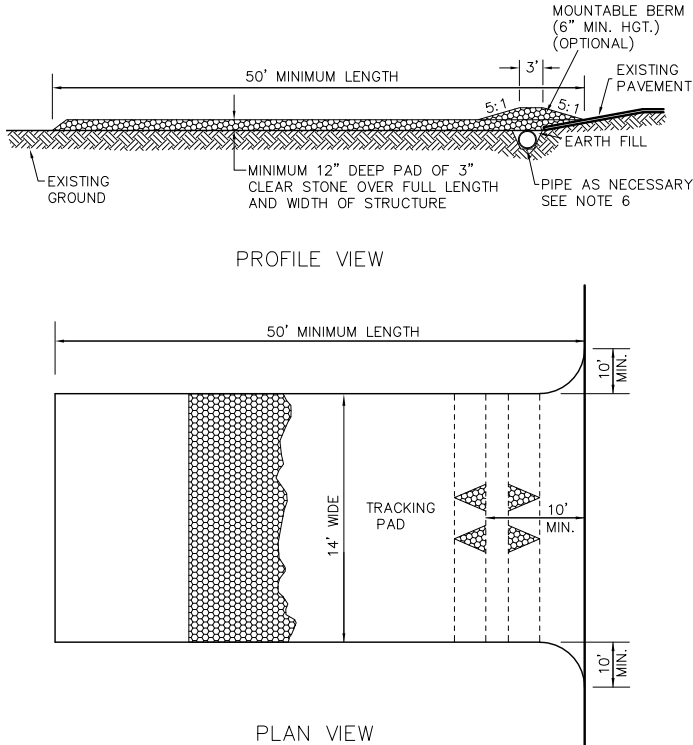
- THE CONTRACTOR SHALL BOLT THE PIPE GATE TO THE CONCRETE ENDWALL WITH FOUR 3/8"x6" MACHINE BOLTS WITH NUTS ON INSIDE WALL.

PAINTING SPECIFICATIONS:

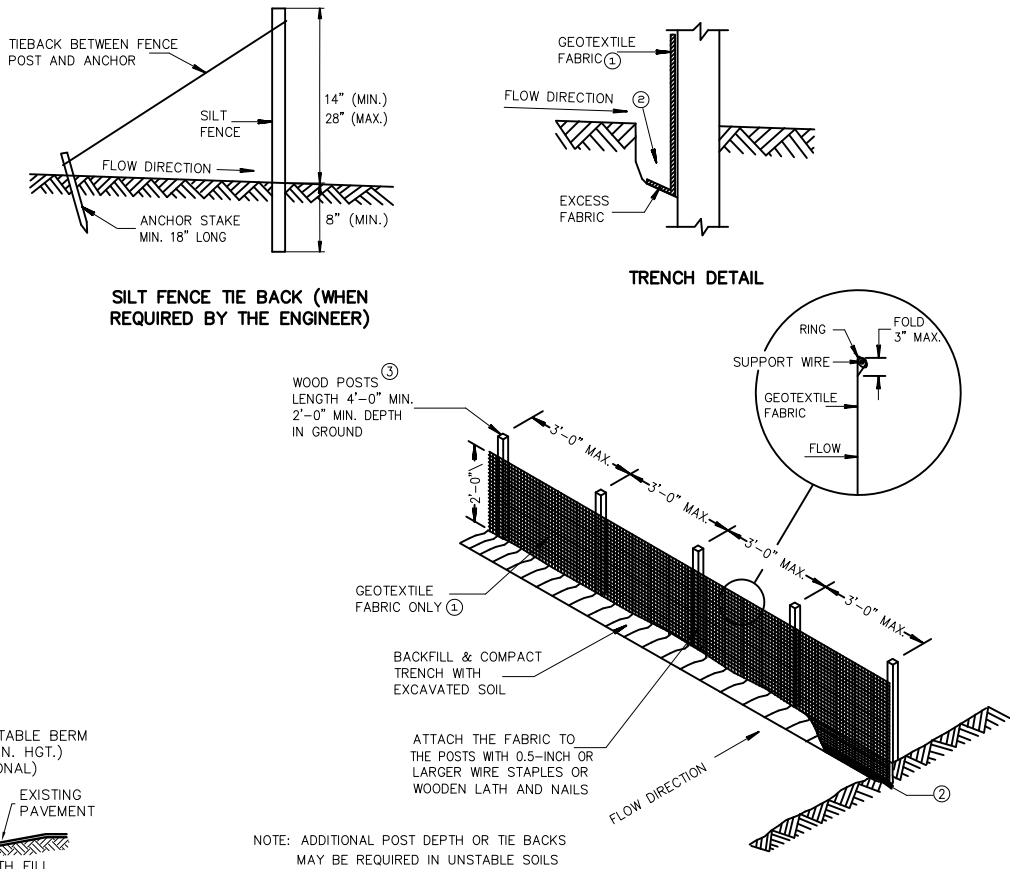
- THE PIPE GATE SHALL RECEIVE THE FOLLOWING PREPARATION & PAINTING. THE FIRST COAT SHALL BE RUS-OLEUM X-60 RED BARE METAL PRIMER OR APPROVED EQUAL. THE SECOND COAT SHALL BE RUS-OLEUM 960 ZINC CHROMATE PRIMER OR APPROVED EQUAL. THE THIRD COAT SHALL BE RUS-OLEUM 1282 HIGH GLOSS METAL FINISH OR APPROVED EQUAL.

PREPARATION STEPS:

- BARE METAL SURFACES - TREAT WITH THE THREE-COAT PAINTING SYSTEM LISTED AFTER A THOROUGH SCRAPING, WIRE BRUSHING & CLEANING.
- EACH COAT OF PAINT SHALL BE APPLIED OVER THE ENTIRE GATE SURFACE.
- ALLOW 24-48 HOURS DRYING TIME AT 60° OR ABOVE BETWEEN COATS.

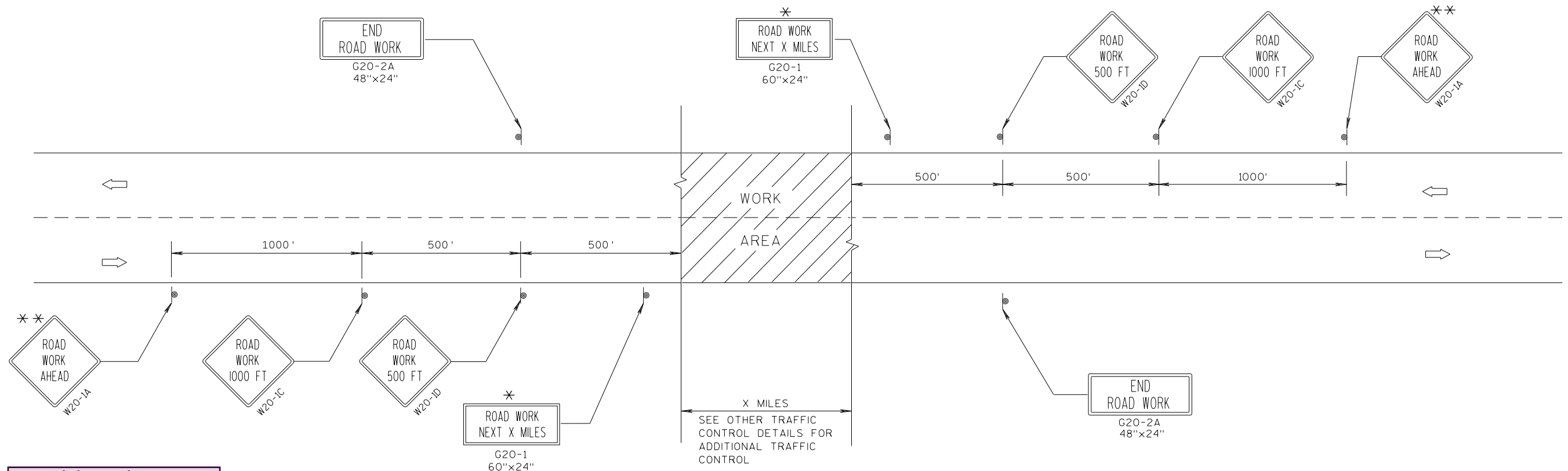


- FOLLOW WISCONSIN DNR TECHNICAL STANDARD 1057 FOR FURTHER DETAILS AND INSTALLATION.
- LENGTH - MINIMUM OF 50'.
- WIDTH - 24' MINIMUM, SHOULD BE FLARED AT THE EXISTING ROAD TO PROVIDE A TURNING RADIUS.
- ON SITES WITH A HIGH GROUND WATER TABLE OR WHERE SATURATED CONDITIONS EXIST, GEOTEXTILE FABRIC SHALL BE PLACED OVER EXISTING GROUND PRIOR TO PLACING STONE. FABRIC SHALL BE WISDOT TYPE-HR GEOTEXTILE FABRIC.
- STONE - CRUSHED 3" CLEAR STONE SHALL BE PLACED AT LEAST 12" DEEP OVER THE ENTIRE LENGTH AND WIDTH OF ENTRANCE.
- SURFACE WATER - ALL SURFACE WATER FLOWING TO OR DIVERTED TOWARDS CONSTRUCTION ENTRANCES SHALL BE PIPED THROUGH THE ENTRANCE. MAINTAINING POSITIVE DRAINAGE. PIPE INSTALLED THROUGH THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE PROTECTED WITH A MOUNTABLE BERM WITH 5:1 SLOPES AND MINIMUM OF 6" STONE OVER THE PIPE. PIPE SHALL BE SIZED ACCORDING TO THE DRAINAGE REQUIREMENTS. WHEN THE ENTRANCE IS LOCATED AT A HIGH SPOT AND HAS NO DRAINAGE TO CONVEY A PIPE SHALL NOT BE NECESSARY. THE MINIMUM PIPE DIAMETER SHALL BE 6". CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF SAID PIPE.
- LOCATION - A STABILIZED CONSTRUCTION ENTRANCE SHALL BE LOCATED WHERE CONSTRUCTION TRAFFIC ENTERS AND/OR LEAVES THE CONSTRUCTION SITE. VEHICLES LEAVING THE SITE MUST TRAVEL OVER THE ENTIRE LENGTH OF THE TRACKING PAD.





SDD 15c4 Traffic Control, Advance Warning Signs 45 M.P.H. or Greater, Two Way Undivided Road Open to Traffic



at a minimum lay your signs out to see where they land, NB traffic may have sight distance issues due to the hill.

TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

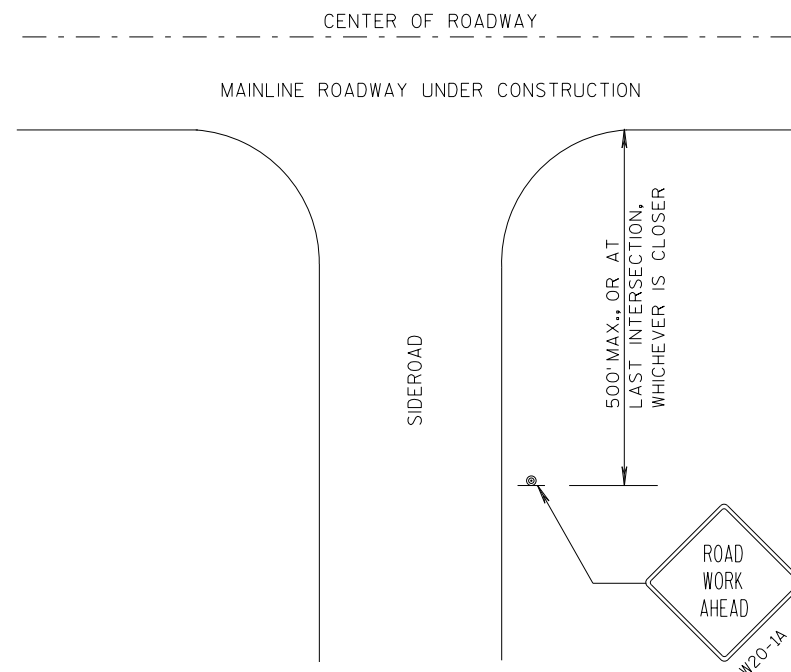
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

* * PLACE ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

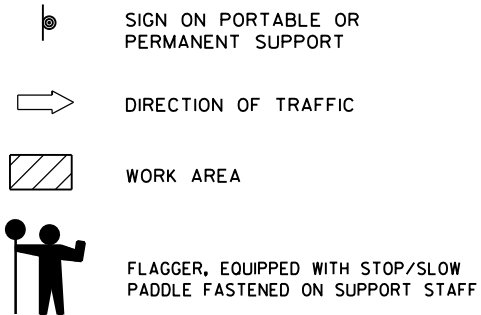


LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND



SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

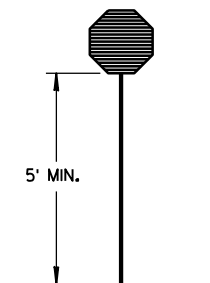
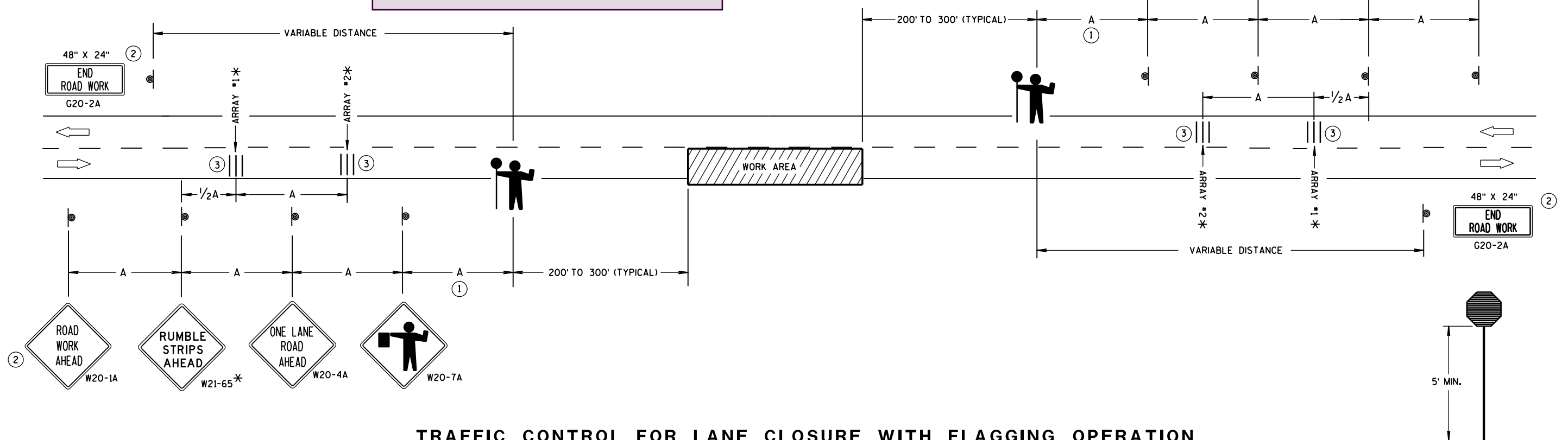
SPEED LIMIT	SPACING A
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING A.

Not the most current SDD - only one TPRS is required.

Not sure a SDD is appropriate for this area due to turn lanes and islands. I would show out a TC plan sheet or two.



STOP/SLOW PADDLE ON SUPPORT STAFF

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"W0" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS. PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* UTILIZE TEMPORARY RUMBLE STRIPS WHEN FLAGGING OPERATION IS ANTICIPATED TO BE STATIONARY IN EXCESS OF TWO HOURS.

- FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS (IF USED) SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- EACH TEMPORARY RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heldtke
DATE WORK ZONE ENGINEER
FHWA



15D28: Traffic Control, Work on Shoulder or Parking Lane, Undivided Roadway

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

TABLE A

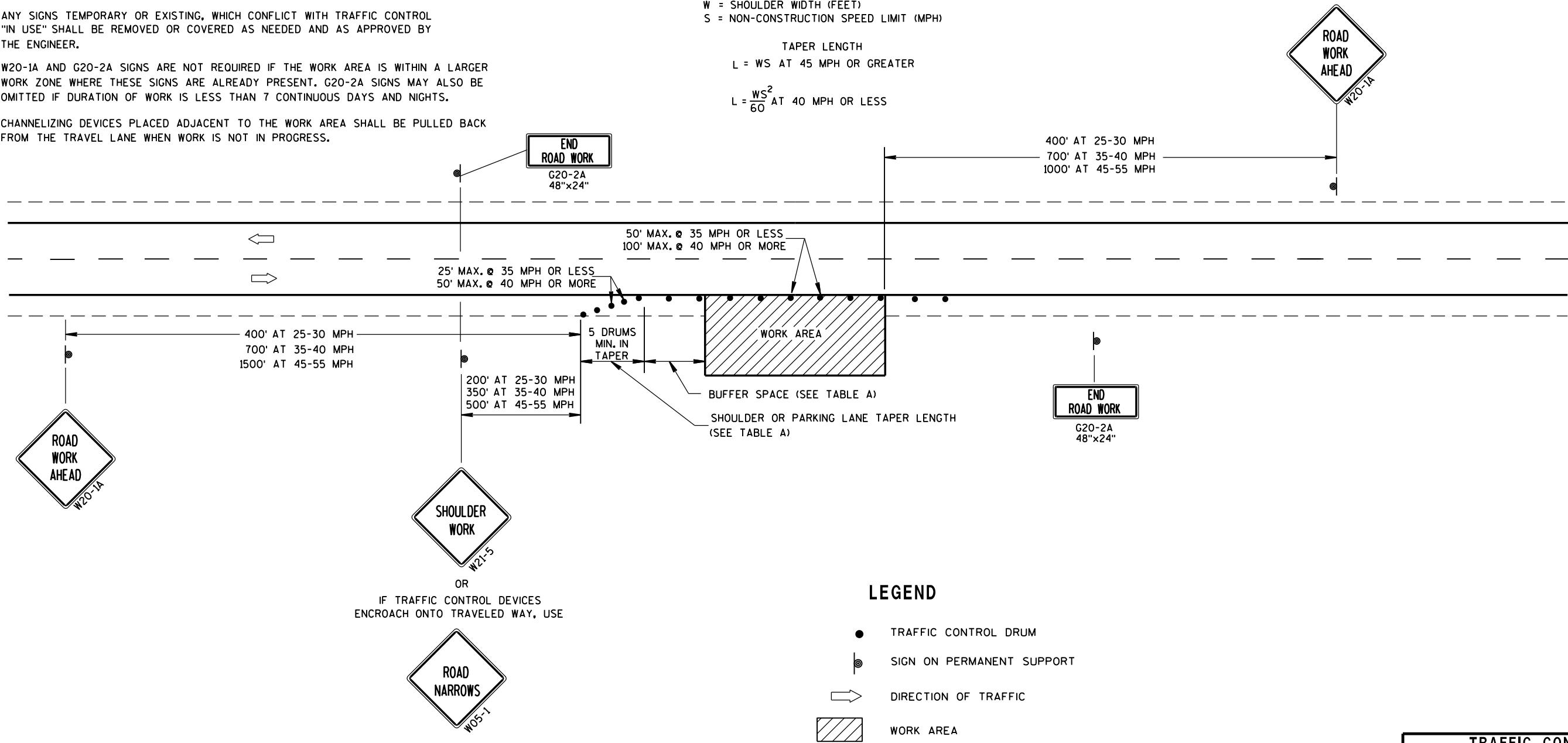
SHOULDER TAPER LENGTH (FEET)					BUFFER SPACE (FEET)
S	W	4	6	8	
30	20	30	40	50	200
35	30	45	55	70	250
40	40	55	75	90	305
45	60	90	120	150	360
50	70	100	135	170	425
55	75	110	150	185	495

W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH
L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = $\frac{1}{3}L$



LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

TRAFFIC CONTROL,
WORK ON SHOULDER OR
PARKING LANE,
UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 14, 2015 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER
FHWA