

APPLICATION/PERMIT FOR CONNECTION TO STATE TRUNK HIGHWAY

Wisconsin Department of Transportation (WisDOT)
DT1504 9/2009 s. 86.07(2) Wis. Stats. & Ch. Trans 231 Wis. Adm. Code

- This form is an application for permission for a STH Connection. If approved, the permit does **not** create a **right of access** or **access easement** for the property.
- If the property has any legal restrictions that prohibit access to a state trunk highway (STH), WisDOT cannot approve a STH connection permit for the property.
- Please type or print neatly in ink.** Fill out the required information on **both sides** of this form per the instructions. One form is required per connection. For more information, visit: <http://wisconsindot.gov/Pages/doing-bus/real-estate/permits/default.aspx>
- Please provide a copy of the property deed with all applications.**

WISDOT OFFICE
INFORMATION

Document Identification Number: **13-100332405-2024
AMEND-1**

Wisconsin DOT
SW Region - Madison Office
2101 Wright St.
Madison, WI 53704

APPLICANT INFORMATION	1. Applicant Name and Mailing Address – Street/PO Box, City, State, ZIP Code Sky Ridge-Sun Prairie, LLC 230 Ohio St. - Suite 200 Oshkosh, WI 54902			2. Property Owner Name / Address (If not applicant)	
				3. If Not Property Owner, Reason for Application	
CONNECTION LOCATION INFORMATION	4. Highway Number(s) 19	5. County Dane	6. <input checked="" type="checkbox"/> City <input type="checkbox"/> Village <input type="checkbox"/> Town of: Sun Prairie	7. Side of the Highway <input checked="" type="checkbox"/> North <input type="checkbox"/> South <input type="checkbox"/> East <input type="checkbox"/> West	
	8. Located within the SW Quarter, of the SE Quarter, Section 04, Town 08N North, Range 11E			9. Fire or Street Number (If applicable) 1050	
	10. Name of Nearest Side Road from Location Distance and Direction from Side Road Musket Ridge Dr. 1,150 1,000 ft East (Feet or Miles) (N, S, E, W)			11. How far is the location from the nearest non-side road connection on the same highway? 350 600 ft (Feet or Miles)	
	Latitude: 43°10'51.1"N Longitude: 89°11'39.0"W				
CONNECTION TYPE INFORMATION	12. Proposed Activity (Check one) <input checked="" type="checkbox"/> Construct New <input type="checkbox"/> Alter Existing <input type="checkbox"/> Remove Existing <input type="checkbox"/> Permit Existing	13. Proposed Use (Check one – See instructions for category description) <input type="checkbox"/> Urban – Commercial/Industrial <input type="checkbox"/> Rural – Agricultural <input type="checkbox"/> Urban – Residential <input type="checkbox"/> Public Road <input checked="" type="checkbox"/> Rural – Commercial/Industrial <input type="checkbox"/> Trail or Trail Crossing <input type="checkbox"/> Rural – Residential Is this a change of the existing use (If applicable)? <input type="checkbox"/> Yes <input type="checkbox"/> No		16. Proposed Trips Per Day (Check one) <input type="checkbox"/> Seasonal <input type="checkbox"/> 101-1000 <input type="checkbox"/> 1-50 <input checked="" type="checkbox"/> Over 1000 <input type="checkbox"/> 51-100 Peak hour traffic count: <u>95</u>	
	14. Proposed Width: 32 ft		15. Proposed Surface: Asphalt		QUESTIONS 17-27 on BACK PAGE

All connections have restrictions, which include the information preprinted and filled-in on this application along with any conditions, covenants, supplemental provisions, superimposed notes, and detail drawings added by WisDOT. The applicant shall be responsible for complying with these restrictions along with the construction and maintenance of the connection. It is further understood that WisDOT's approval is subject to applicant's full compliance with relevant state statutes and administrative rules, plus any laws, codes, rules, regulations, ordinances and permit requirements of other jurisdictional agencies. The alteration of this form by the applicant is prohibited. Under s. 86.073 Wisconsin Statutes, WisDOT's permit denial or revocation may be appealed.

The applicant affirms that all information provided on and submitted with this form is correct and complete. Providing incorrect or incomplete information may result in permit denial or revocation as well as criminal prosecution under s. 946.32 Wisconsin Statutes.

X *Sean OB* 01/24/2024
(Property Owner or Authorized Representative) (Date)

608-334-5665 sean@northpointedev.com
(Area Code and Telephone #) (E-mail address if available)

FOR OFFICIAL WISDOT USE ONLY – DO NOT WRITE BELOW THIS LINE

☒ THE FOLLOWING SUPPLEMENTAL PROVISIONS APPLY

- Please contact WisDOT Pete Kaiser 608.245.2650 7 days prior to work within the Right of Way (ROW).
- Follow plans from 11-07-2024
- Provide a schedule prior to work on ROW
- If closing a lane or shoulder, provide the following advance notification to the Wisconsin Lane Closure System (<https://transportal.cee.wisc.edu/closures/>)

- See additional Supplemental Provisions.
- Amendment #1 November 12, 2024 (Updated Bike Path Width Plans)

<input checked="" type="checkbox"/> Permit APPROVED (THIS PERMIT IS REVOCABLE)	
<input type="checkbox"/> Permit DENIED	
<input type="checkbox"/> Application DISMISSED	
X <i>Arthur P. Lamm</i>	11/07/2024
(WisDOT Authorized Representative) (Date)	
(If Computer-filled, Brush Script Font)	
<input type="checkbox"/> Temporary permit. Expiration date:	
<input type="checkbox"/> This permit voids / replaces permit #	
<input type="checkbox"/> Shared connection	Co-user name:
	Related permit #

File:

17. ☒ Yes ☐ No Is the property zoned? Provide documentation from the jurisdictional zoning authority.

18. Explain how the land is currently being used:

Lot is zoned as Planned Development (PD). lot is currently vacant. No zoning change planned.

19. ☐ Yes ☒ No Are you aware of any plan to change the zoning or land use for the property? If yes, explain.

20. ☒ Yes ☐ No Is this connection for property that was part of a land division or assemblage created on or after 2/1/99? If yes, explain.

Lot 1 of CSM 16354 dated 09/26/2023

21. ☐ Yes ☒ No Do you own other property abutting the highway that is next to the property that this connection will serve? If yes, describe.

Note 1: Answering "yes" to #20 or 21 may result in a Wis. Administrative Code Trans 233 review by WisDOT. If this is needed, the processing of your application may be delayed.

22. ☐ Yes ☒ No Does the property abut or border another public road? If yes, provide the road's name.

23. ☐ Yes ☒ No Are there any existing connections to any road serving the property? If yes, how many? _____

24. ☐ Yes ☒ No Are there any restrictions on the number, use, or location of connections to the property?

25. ☒ Yes ☐ No Are there any access easements across the property (recorded or unrecorded)?

Note 2: If you answered "yes" to #24 or 25, provide a copy of **all documents** relevant to the restrictions and/or easements

26. Provide the property tax identification number: 2282/0811-044-6601-2

27. What is the proposed construction completion date for the connection? See Conditions of Issuance #9. ~~02/01/2025~~ **06-15-2025**

FOR ANY NEW CONNECTION OR CONNECTION TO BE MOVED, APPLICANT SHALL PLACE A FLAG OR MARKER IN THE HIGHWAY DITCH OR FORESLOPE (VISIBLE FROM THE HIGHWAY) AT THE PROPOSED LOCATION.

CONDITIONS OF ISSUANCE

1. WisDOT reserves the right to make such changes, additions, repairs, relocations and removals within statutory limits to the connection or its appurtenances on the right-of-way as may at any time be considered necessary to facilitate relocation, reconstruction, widening, and maintenance of the highway, or to provide proper protection to life and property on or adjacent to the highway, or if alternative access to the property becomes available.

2. Except in cases where the indicated connection may be constructed by forces acting on behalf of WisDOT due to a highway improvement or maintenance project, the permittee shall do all of the following: (a) Furnish all materials, do all work, and pay all costs for the construction and maintenance of the connection and its appurtenances on the right-of-way. Materials used and the type and quality of the work shall be appropriate for the connection type specified in this permit; (b) Do all work without jeopardy to, or interference with, traffic using the highway; (c) Promptly restore to WisDOT's satisfaction and at least pre-existing conditions, highway surfaces, shoulders, ditches, vegetation, drainage appurtenances, guardrail, signs, electric conduits/cables, etc. disturbed by any work; (d) Maintain the connection and its appurtenances within the right-of-way limits, which includes keeping them in a proper state of repair, preventing the blockage or impairment of right-of-way drainage, and preventing aboveground obstacles to occur that could cause serious injury or death to a motorist in an errant vehicle; (e) Not disturb or cause the disturbance of any survey marker without the **prior written approval** of WisDOT.

3. The permittee, indicated on the reverse side, represents all parties in interest, and agrees that any connection or approach constructed by or for the permittee is for the purpose of obtaining access to the property (listed in #26) and not for the purpose of parking or servicing vehicles, or for advertising, storage, or merchandising of goods on the right-of-way.

4. The permittee shall not make any changes to the connection, its appurtenances, or adjacent right-of-way, including, without limitation, changes to the location (#4-11), use (#13), width (#14), surface (#15), or increase the number of trips per day (#16) approved by this permit without obtaining **prior written approval** from WisDOT. WisDOT will evaluate all potential highway impacts that may arise from the proposed change(s) and therefore, does not guarantee approval.

5. The permittee, successors or assigns agree to hold harmless the State of Wisconsin and its duly appointed agents and employees against any action for personal injury or property damage sustained by reason of the exercise of this permit.

6. WisDOT does not assume any responsibility for the removal or clearance of snow, ice or sleet, or the opening of windrows of such material upon any portion of any connection along any state trunk highway even though snow, ice or sleet is deposited or windrowed on said connection by its authorized representative engaged in normal winter maintenance operations.

7. Under Wisconsin Administrative Rule Trans 401, the permittee shall implement proper erosion control and storm water management measures at all times during work operations and upon completion of the connection to protect all restored areas until the replacement vegetation achieves sustained growth.

8. Rock, asphalt, concrete, timber or other embankment retention or marking treatments for the connection **are prohibited**.

9. If the permitted work has not been started by the construction completion date, this permit is **null and void**. If the work has been started but is not finished by the construction completion date, no additional work shall be done unless authorized through an approved written time extension or a subsequent permit from WisDOT. Accomplishment of any part of the work shall verify agreement by the permittee to conform to this permit and to be bound by all of its requirements, restrictions, and obligations, all of which shall continue to be in full force and effect. WisDOT does not guarantee the approval of a time extension or subsequent permit since the conditions WisDOT based its permit approval upon may have changed between the permit approval date and time extension request date.

Instructions for APPLICATION/PERMIT FOR CONNECTION TO STATE TRUNK HIGHWAY

Wisconsin Department of Transportation (WisDOT) form DT1504 (9/2009)

GENERAL INSTRUCTIONS

- ☐ Use one form for each state trunk highway (STH) connection. A connection can be a driveway, public or private road, or a trail or trail crossing. The form initially serves as an **application** for a connection between a property and a STH. Then if approved, the completed form becomes a **permit** for the STH connection. If the property has any legal restrictions that prohibit access to a STH, WisDOT cannot approve a STH connection permit for the property, and therefore, you should not apply for this permit.
- ☐ Two copies of this application must be completed, signed and dated by the applicant. Send one copy with an original signature to the appropriate Regional Transportation Office, and **keep** the other for your records. Go to: <http://wisconsindot.gov/Pages/doing-bus/real-estate/permits/default.aspx> for a list of office addresses, staff contacts, and a regional boundary map.
- ☐ Just below the signature line, provide an area code and telephone number where you can be called between the hours of 8 a.m. and 4:30 p.m. Monday through Friday. Note at the top of the form if there are particular times of the day and/or days of the week that are more convenient to call you. If you have an e-mail address, provide that too. WisDOT may send you correspondence and/or your approved permit via e-mail in order to speed up the processing of your application.
- ☐ Fill out all required information. Processing may be delayed or a form returned if information is missing.
- ☐ **With your application, provide a copy of the property deed.** Also provide a copy of a certified survey map, if applicable.
- ☐ If you have an existing WisDOT permit for a connection that you are planning to alter, relocate, or remove, provide a copy of that permit.
- ☐ Copies of Wisconsin Statute 86.07(2) and Administrative Code Trans 231, which regulate a STH connection, are either enclosed (if this form was mailed to you) or available on the website listed above. Contact your local government officials for other statutes, ordinances, and permit requirements that may also apply to connections.

SPECIFIC INSTRUCTIONS FOR EACH QUESTION

1. **Applicant Name and Mailing Address:** Provide the full name of the property owner or person requesting the permit. For a public road, this must be a unit of government. Also provide the street name and number and/or PO box number, city, state, and ZIP code. The address information is used to return the application/permit to the applicant.
2. **Property Owner Name / Address (if not applicant):** Provide the full name and address.
3. **If Not Property Owner, Reason for Application?** Permits are typically issued to property owner(s), but may be issued to prospective buyers. For example, if you are not the current property owner but plan on purchasing the property, then provide a copy of the **accepted** "Offer to Purchase" document with your application.
4. **Highway Number(s):** Provide the STH route number that the connection will attach to. Provide all route numbers if two or more STHs run concurrently.
5. **County:** Provide the county name.
6. **City - Village - Town:** Check the appropriate box and provide the name.
7. **Side of the Highway:** Check the appropriate box for the side of the highway of the connection location.
8. **Located Within the:** This information may be obtained from the property tax bill or by contacting the appropriate town, village, city, or county clerk. If you cannot obtain this information, leave the section blank and WisDOT will complete it. Choices for quarter are: NE, NW, SE, or SW; for Section: 1-36; for Town: 1-51; for Range: 1-20 W or 1-29 E.

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9. **Fire or Street Number (if applicable):** Provide the fire or street number if one has been assigned to the property, or provide it to WisDOT as soon as it is assigned. A Government Lot number may be inserted if there is no fire or street number.

10. **Name of Nearest Side Road from Location:** Provide the full name of the nearest intersecting road to the connection.

Distance and Direction from Side Road: Estimate the approximate distance (in feet or miles) by starting at the nearest side road and going along the STH until you reach the center of the connection location. Select north, south, east or west (or N, S, E, W) for the direction.

11. **How far is the location from the nearest non-side road connection on the same highway?** A non-side road connection will either be a driveway or a trail crossing, and may be on either side of the highway. For questions 10 and 11, the distance may be estimated by driving (0.1 mile equals 528 feet).

12. **Proposed activity:** Check only **one** of the four activity boxes:
 - ☐ **Construct New:** to construct a new connection. This may include removal of existing connections if required. If you are relocating a connection with a valid WisDOT permit, submit **one** form with "construct new" checked, and WisDOT will generate a corresponding form with "remove existing."
 - ☐ **Alter Existing:** to physically modify, or change the use* of, a connection with a valid WisDOT permit whose location will not change. This includes work beyond routine maintenance such as paving a connection, widening a connection, replacing a culvert, and improvements to the highway.
 - ☐ **Remove Existing:** to remove a legal connection.
 - ☐ **Permit Existing:** to obtain a valid WisDOT permit for an existing unpermitted connection.

*Note: "Change the use", means any modification to a property that results in a change in the number and/or types of vehicles using a property's connection. Examples include changing from residential to commercial use, changing from single to multiple residential use, subdividing to accommodate additional residences or businesses, and changing the type of business.

13. **Proposed use:** Each connection serves a specific use. Check the **one** box that **best** defines the use. A rural-type highway typically has a ditch. An urban-type highway typically has curb and gutter.
 - ☐ **Rural - Commercial/Industrial:** a retail, wholesale, industrial or non-profit business next to a rural-type highway. *35' maximum*
 - ☐ **Rural - Residential:** a private home next to a rural-type highway. *16' minimum - 24' maximum*
 - ☐ **Rural - Agricultural:** a field for planting, maintaining, and/or harvesting crops or tending livestock, or land for recreational and hunting use, next to a rural-type highway. This use may serve farm buildings, but may not serve residential buildings. *16' minimum - 24' maximum*
 - ☐ **Urban - Commercial/Industrial:** a retail, wholesale, industrial or non-profit business next to an urban-type highway. *35' maximum*
 - ☐ **Urban - Residential:** a private home next to an urban-type highway. *24' maximum*
 - ☐ **Public Road:** a road, street, highway, etc. that connects to a STH for public travel and use and is maintained by a local unit of government.
 - ☐ **Trail or Trail Crossing:** a bike, snowmobile, etc. trail that is parallel to and/or crosses a STH.

Is this a change of the existing use (if applicable)? Check yes or no. See the note in #12 for the definition of "Change the Use".

14. **Proposed Width:** Provide the proposed width of the connection as measured at the right of way line. Minimum and maximum widths are regulated in Trans 231 and are listed in *italics* in #13.

15. **Proposed Surface:** Choose from grass, gravel, asphalt, or concrete.

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16. **Proposed Trips Per Day:** Estimate how many trips will be made daily by vehicles going in and out of the connection. One vehicle going in and out of the connection equals two trips. "Seasonal" means a connection that is only used a few times during a year, for example, a field entrance or entrance to hunting lands.

Peak hour traffic count: For any connection that will generate 100 or more trips in any hour, 50 or more trips exiting in one hour, or 750 trips in an average day, provide the number of vehicles using the connection during the peak hour.
17. **Is the property zoned?** Check yes or no. **In either case, provide the appropriate documentation**, which may be obtained from the local zoning administrator or clerk of the appropriate township, village, city, or county where the property is located. Typically, a simple statement from the authority on its letterhead is sufficient for documentation, or WisDOT can provide a form if needed.
18. **Explain how the land is currently being used:** If the land use and zoning are the same, then write "same as zoning". If not, please explain the use. You may use the descriptions listed in #13 as a guide.
19. **Are you aware of any plan to change the zoning or land use for the property?** Check yes or no. **If yes, briefly explain** the circumstances regarding the change. For example, "construct new house" or "proposed development."
20. **Is this connection for a property that was part of a land division or assemblage created on or after 2/1/99?** Check yes or no. **If yes, provide a brief explanation.** For example, if the connection is for property that is being created from a larger piece of property, check yes. A land division can be splitting one property into two or more properties, or it can be an assemblage, which is the joining of properties or portions of properties to make a different property.
21. **Do you own other property abutting the highway that is next to the property that this connection will serve?** Check yes or no. **If yes, briefly describe.** The land may abut either side of the highway.
22. **Does the property abut or border another public road?** Check yes or no. **If yes, provide the road's name.**
23. **Are there any existing connections to any road serving this property?** Check yes or no. The road may be public or private. **If yes, how many?** Provide the total number including any field. Field entrances should be included in this amount.
24. **Are there any restrictions limiting the number, use, or location of connections to the property?** Check yes or no. See #25.
25. **Are there any access easements across the property?** Check yes or no. Access easements and/or access restrictions may be documented on a: subdivision plat, certified survey map, deed, access covenant (recorded or unrecorded), agreement, plat, map, or existing permit (WisDOT or non-WisDOT issued). Carefully review these documents when applying for a STH connection permit. An access easement is typically defined as granting another party the right of vehicular access across a property.
26. **Provide the property tax identification number:** This information may be found on the property tax bill or a county (or city) land records web site.
27. **What is the proposed construction completion date for the connection?** Provide the date. The date should not be longer than **one year** from the permit issuance date. If the permitted work has not been started by the date, the permit is **null and void**. If the permitted work has started but is not finished by the date, no additional work can be done unless an approved written time extension or a subsequent permit is obtained from WisDOT. To request either one, contact the WisDOT office that issued the current permit. Time extensions and subsequent permits are not automatically approved. A field review may be required to determine if conditions have changed since the current permit was approved. WisDOT strongly recommends completing construction of an approved connection as quickly

**Instructions for
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Wisconsin Department of Transportation (WisDOT) form DT1504 (9/2009)
as possible. Once construction has started, it should be completed within 30 days to minimize traffic disruption.

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If the connection is a public road:

- ☐ The applicant must be the appropriate unit of government.
- ☐ Provide a copy of the:
 - Resolution from the appropriate government Board or Council documenting the approval of the proposed road.
 - Executed deed showing the transfer of property from the landowner to the appropriate unit of government.

For all public and private roads:

- ☐ The proposed road must conform to local road standards as found in Wisconsin Statutes and WisDOT's Facilities Development Manual.
- ☐ Provide a copy of the:
 - General location map. An existing plat book map may be used for this.
 - Plat, if applicable
 - Traffic Impact Analysis (TIA), if required by WisDOT or local unit of government
 - Pictures of the proposed intersection from all four directions
- ☐ Provide highway plans for the STH and proposed road showing the:
 - Designated right-of-way widths (See [s. 82.50 Wisconsin Statutes](#) for Town Road Standards)
 - Land ties and/or reference points
 - Proposed geometrics including surface type(s), lane width(s), approach grades (in all directions), turn lanes, bypass lanes, return radii, etc.
 - Drainage patterns and drainage structures including culvert size, endwalls, and extensions, if needed
 - Closest connections (other roads, driveways, trail crossings) in both directions along the STH from the proposed road, and along the proposed road from the STH
 - Cross-sections of all road improvements every 50' showing the original ground, proposed finished pavement elevations, ditches, and right-of-way line. Cross-sections along the local road should be provided at least 100' from the STH right-of-way line.
 - Pavement marking plan including new or restored markings, if needed
 - Erosion control and restoration plans
 - Traffic control plans during construction

For all private roads:

- ☐ Provide a copy of:
 - Proof of insurance, if requested by WisDOT.
 - A road maintenance agreement, if such an agreement has been developed and implemented by a group of property owners, businesses, etc., or association.

If the connection is a trail or trail crossing:

- ☐ The applicant may be a property owner, recreational group (for example, a snowmobile club), or unit of government.
- ☐ For a trail crossing in which you are not the property owner, provide written permission from the property owners on each side of the highway that the connection can be attached to their property.
- ☐ Provide a copy of the resolution or agreement that details the maintenance of the trail.

SUPPLEMENTAL PROVISIONS

STH 19 Sky Ridge / Roers Development., City of Sun Prairie, Dane Co.

Pursuant to Wisconsin Statutes and once approved by WisDOT, this permit allows performance of the specific work described over which WisDOT has permit authority. ***The permittee shall abide by these general provisions, and any supplemental and/or special provisions.*** (ROW = right-of-way)

- Please contact Pete Kaiser (WisDOT) 608.245.2650 7 days prior to work within the Right of Way.
- Provide a schedule prior to work on ROW.
- Provide the WDNR and/or Municipal/County Construction Permit.
- Follow Plans dated November 5, 2024 (Updated).
- **WORK RESTRICTIONS:** Daily, holiday and/or seasonal work restrictions apply to the permitted work as detailed:
 - From noon Wednesday, November 27, 2024 to 6:00 AM Monday, December 2, 2024, for Thanksgiving;
 - From noon Monday, December 23, 2024 to 6:00 AM Thursday, December 26, 2024, for Christmas;
 - From noon Tuesday, December 31, 2024 to 6:00 AM Thursday, January 2, 2025, for New Years;
 - From noon Friday, May 23, 2025 to 6:00 AM Tuesday, May 27, 2025 for Memorial Day;
 - From noon Thursday, July 3, 2025 to 6:00 AM Monday, July 7, 2025 for Independence Day;
 - From noon Thursday, August 14, 2025 to 6:00 AM Monday, August 18, 2025 for Sun Prairie Sweet Corn Festival.
 - From noon Friday, August 29, 2025 to 6:00 AM Tuesday, September 2, 2025 for Labor Day.
- **Traffic Management:**
 - FINAL Traffic Management Plan completed 10-14-2024.
 - If a lane of traffic in each direction cannot be maintained, then contact WisDOT work zone coordinator Lee Gibbs 608-206-6428 to provide best traffic management practices.
 - Signage should follow Wisconsin Manual on Uniform Traffic Control Devices.
 - If closing a lane or shoulder, Provide the following advance notification to the engineer for incorporation into the Wisconsin Lane Closure System (LCS). (<https://transportal.cee.wisc.edu/closures/>)
 - Any highway open to traffic that has an aggregate surface must be repaved with HMA within 72 hours or by the end of the week whichever occurs first.
 - Flaggers, if used, must be certified.
 - Do not store equipment or materials within 10' of traffic.
 - Any drop-off greater than 2" needs to be protected by a 3:1 slope or temporary concrete barrier is required.

Wisconsin Lane Closure System Advance Notification:

TABLE 108-1 CLOSURE TYPE AND REQUIRED MINIMUM ADVANCE NOTIFICATION

Closure type with height, weight, or width restrictions (available width, all lanes in one direction < 16')	MINIMUM NOTIFICATION
Lane and shoulder closures	7 calendar days
Full roadway closures	7 calendar days
Detours	7 calendar days
Closure type without height, weight, or width restrictions (available width, all lanes in one direction ≥ 16')	MINIMUM NOTIFICATION
Lane and shoulder closures	3 business days
Modifying all closure types	3 business days

- **Utilities:** Avoid or move utilities that may be in the way of the work in right of way.
- **Erosion Control:**
 - Do not store any materials within the State ROW.
 - Implement erosion control best management practices (BMPs) prior to and at all times during work operations. Provide and maintain erosion control BMPs to protect all restored areas upon completion of the permitted work until the replacement vegetation achieves sustained growth.
 - Any sediment that is deposited from the site onto WisDOT facilities needs to be removed and the area stabilized within 24 hours of the release.
 - Any sediment or materials tracked onto state right of way needs to be cleaned within that workday.
 - The applicant will assure that the ditches/storm sewer (down-stream) are cleaned and positively flow, prior to finalizing the construction.
 - Temporary erosion control best management practices need to be removed and the area restored.
 - After temporary use is complete, the area will be restored to pre-construction conditions in accordance with applicable parts of [WisDOT's Standard Specifications for Highway and Structure Construction](#), current edition.
- **The final restoration work needs review prior to acceptance.**

GENERAL NOTES:

- THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE CAUSED DURING CONSTRUCTION TO PUBLIC PROPERTY, PRIVATE PROPERTY OR UTILITIES.
- THE CONTRACTOR SHALL SUBMIT TWO COPIES OF SHOP DRAWINGS FOR REVIEW BY THE ENGINEER, PRIOR TO PLACING AN ORDER OF ANY SUCH ITEM.
- EXISTING TOPOGRAPHIC INFORMATION IS BASED ON FIELD OBSERVATIONS AND/OR PLAN OF RECORD DRAWINGS. CONTRACTOR SHALL VERIFY TOPOGRAPHIC INFORMATION PRIOR TO STARTING CONSTRUCTION.
- RIGHT OF WAY (ROW) AND PROPERTY LINES ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXISTING PROPERTY CORNER MONUMENTATION. ANY MONUMENTS DISTURBED BY CONTRACTOR SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
- CONTRACTOR SHALL COORDINATE WITH DRY UTILITY COMPANY'S REGARDING ANY POTENTIAL CONFLICTS AND COORDINATE RELOCATIONS AS MAY BE REQUIRED. CONTRACTOR SHALL ALSO COORDINATE THE PROPOSED INSTALLATION OF NEW FACILITIES AS REQUIRED.
- NO IMPERVIOUS SURFACES TO BE INSTALLED PRIOR TO CONSTRUCTION OF AREA H BASIN BY THE CITY OF FITCHBURG.
- THE CITY OF SUN PRAIRIE STANDARD SPECIFICATION FOR PUBLIC WORKS, 2024 EDITION, INCLUDING DETAILS, SHALL SUPERSEDE SUPPLEMENTARY SPECIFICATIONS AND DETAILS.

EXISTING CONDITIONS NOTES:

- THE PROPERTY LINES SHOWN ON THIS MAP ARE BASED ON FOUND PROPERTY CORNERS AND SURVEYS OF RECORD.
- THIS MAP IS REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM – DANE COUNTY. ELEVATIONS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88).
- ALL DRY UNDERGROUND UTILITIES HAVE BEEN LOCATED PER MARKINGS PLACED ON THE GROUND. VIERBICHER DOES NOT WARRANT THE LOCATIONS MARKED OR MAPPED BY OTHERS.
- SANITARY SEWER UTILITY LOCATIONS ARE BASED ON SURVEYED STRUCTURES.
- STORM SEWER UTILITY LOCATIONS ARE BASED ON SURVEYED STRUCTURES.
- WATER MAIN LOCATIONS ARE BASED ON SURVEYED STRUCTURES.

DEMOLITION NOTES:

- CONTRACTOR SHALL KEEP ALL CITY STREETS FREE AND CLEAR OF CONSTRUCTION RELATED DIRT/DUST/DEBRIS.
- COORDINATE EXISTING UTILITY REMOVAL/ABANDONMENT WITH LOCAL AUTHORITIES AND UTILITY COMPANIES HAVING JURISDICTION.
- ALL SAWCUTTING SHALL BE FULL DEPTH TO PROVIDE A CLEAN EDGE TO MATCH NEW CONSTRUCTION. MATCH EXISTING ELEVATIONS AT POINTS OF CONNECTION FOR NEW AND EXISTING PAVEMENT, CURB, SIDEWALKS, ETC. ALL SAWCUT LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE FIELD ADJUSTED TO ACCOMMODATE CONDITIONS, JOINTS, MATERIAL TYPE, ETC. REMOVE MINIMUM AMOUNT NECESSARY FOR INSTALLATION OF PROPOSED IMPROVEMENTS.
- CONTRACTOR SHALL PROVIDE AND SHALL BE RESPONSIBLE FOR ANY NECESSARY TRAFFIC CONTROL SIGNAGE AND SAFETY MEASURES DURING DEMOLITION AND CONSTRUCTION OPERATIONS WITHIN OR NEAR THE PUBLIC ROADWAY.
- COORDINATE TREE REMOVAL WITH LANDSCAPE ARCHITECT IF APPLICABLE. ALL TREES TO BE REMOVED SHALL BE REMOVED IN THEIR ENTIRETY AND STUMPS SHALL BE GROUND TO 12" BELOW PROPOSED SUBGRADE.
- IF APPLICABLE, PROVIDE TREE PROTECTION FENCING PRIOR TO CONSTRUCTION OPERATIONS. MAINTAIN THROUGHOUT CONSTRUCTION.
- CONTRACTOR SHALL OBTAIN ANY NECESSARY DEMOLITION AND UTILITY PLUGGING PERMITS.
- ANY DAMAGE TO THE CITY PAVEMENT, INCLUDING DAMAGE RESULTING FROM CURB REPLACEMENT, WILL REQUIRE RESTORATION IN ACCORDANCE WITH THE CITY ENGINEERING PATCHING CRITERIA.

SITE PLAN NOTES:

- CONCRETE FOR RAMPS, DRIVEWAYS, AND SIDEWALK AT DRIVEWAY ENTRANCES, SHALL BE 7" THICK, CONSTRUCTED ON A BASE OF 4" COMPACTED SAND OR CRUSHED STONE.
- ALL DIMENSIONS WITH CURB & GUTTER ARE REFERENCED TO THE FACE OF CURB.
- CONTRACTOR SHALL DEEP TILL ANY DISTURBED AREAS AFTER CONSTRUCTION IS COMPLETE AND BEFORE RESTORING.
- CONTRACTOR TO OBTAIN ANY NECESSARY DRIVEWAY CONNECTION, RIGHT OF WAY AND EXCAVATION PERMITS PRIOR TO CONSTRUCTION.
- ANY SIDEWALK AND CURB & GUTTER ABUTTING THE PROPERTY SHALL BE REPLACED IF IT IS DAMAGED DURING CONSTRUCTION OR IF THE CITY ENGINEERING DEPARTMENT DETERMINES THAT IT IS NOT AT A DESIRABLE GRADE, REGARDLESS OF WHETHER THE CONDITION EXISTED PRIOR TO BEGINNING CONSTRUCTION.
- ALL EXISTING SIGNS AND MAILBOXES TO BE MAINTAINED TEMPORARILY AND REINSTALLED AFTER THE PROJECT IS COMPLETE.

GRADING NOTES:

- CONTOURS ARE SHOWN FOR PURPOSES OF INDICATING ROUGH GRADING. FINAL GRADE SHALL BE ESTABLISHED ON PAVED SURFACES BY USING SPOT GRADES ONLY.
- ALL GRADES SHOWN REFERENCE FINISHED ELEVATIONS.
- CROSS SLOPE OF SIDEWALKS SHALL BE 1.5% UNLESS OTHERWISE NOTED.
- LONGITUDINAL GRADE OF SIDEWALK RAMPS SHALL NOT EXCEED 8.33% (1:12) AND SHALL BE IN ACCORDANCE WITH ADA REQUIREMENTS.
- LONGITUDINAL GRADE OF SIDEWALK SHALL NOT EXCEED 5.0% OR THE ADJACENT STREET GRADE WHICHEVER IS GREATER.
- ACCESSIBLE ROUTES SHALL BE 5.0% MAX LONGITUDINAL SLOPE AND 1.5% MAX CROSS SLOPE. ACCESSIBLE LOADING AREAS OR LANDINGS SHALL BE 2.0% MAX SLOPE IN ANY DIRECTION. RAMPS SHALL BE 8.33% MAX SLOPE.
- NO LAND DISTURBANCE ACTIVITIES SHALL BEGIN UNTIL ALL EROSION CONTROL BMP'S ARE INSTALLED.

UTILITY NOTES:

- CONTRACTOR SHALL OBTAIN ANY NECESSARY WORK IN RIGHT OF WAY, EXCAVATION, UTILITY CONNECTION, PLUGGING AND ABANDONMENT PERMITS PRIOR TO CONSTRUCTION.
- CONTRACTOR TO VERIFY EXISTING UTILITY LOCATIONS AND ELEVATIONS PRIOR TO STARTING WORK.
- SANITARY & STORM SEWER LENGTHS SHOWN ARE FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE. STORM SEWER END SECTIONS ARE INCLUDED IN THE LENGTH AND SLOPE OF THE PIPE.
- CONTRACTOR SHALL INVESTIGATE ALL UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY ENGINEER OF ANY CONFLICTS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING ALL UTILITY STRUCTURES TO FINISHED GRADE (MANHOLE RIMS, WATER VALVES, AND CURB STOPS), IF NECESSARY.
- FOR ALL SEWER AND WATER MAIN CROSSINGS: PROVIDE MINIMUM 18" SEPARATION WHEN WATER MAIN CROSSES BELOW SEWER AND MINIMUM 6" SEPARATION WHEN WATER MAIN CROSSES ABOVE SEWER.
- IF DEWATERING OPERATIONS EXCEED 70 GALLONS PER MINUTE OF PUMPING CAPACITY, A DEWATERING WELL PERMIT SHALL BE OBTAINED PRIOR TO STARTING ANY DEWATERING ACTIVITIES.
- A COPY OF THE APPROVED UTILITY PLANS, SPECIFICATIONS AND PLUMBING PERMIT APPROVAL LETTER SHALL BE ON-SITE DURING CONSTRUCTION AND OPEN TO INSPECTION BY AUTHORIZED REPRESENTATIVES OF THE DEPARTMENT OF SAFETY AND PROFESSIONAL SERVICES AND OTHER LOCAL INSPECTORS.
- PROPOSED UTILITY SERVICE LINES SHOWN ARE APPROXIMATE. COORDINATE THE EXACT LOCATIONS WITH THE PLUMBING DRAWINGS. COORDINATE THE LOCATION WITH THE PLUMBING CONTRACTOR AND/OR OWNER'S CONSTRUCTION REPRESENTATIVE PRIOR TO INSTALLATION OF ANY NEW UTILITIES.
- NO PERSON MAY ENGAGE IN PLUMBING WORK IN THE STATE UNLESS LICENSED TO DO SO BY THE DEPARTMENT OF SAFETY AND PROFESSIONAL SERVICES PER S.145.06.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THAT THE EXISTING VALVES WILL HOLD THE PRESSURE TEST PRIOR TO CONNECTION. THE CITY IS NOT RESPONSIBLE FOR ANY COSTS INCURRED DUE TO THE CONTRACTOR NOT VERIFYING THAT THE EXISTING VALVE WILL HOLD THE PRESSURE TEST PRIOR TO CONNECTION. IF A NEW VALVE IS REQUIRED, THE APPLICANT WILL BE REQUIRED TO INSTALL ONE AT THEIR EXPENSE, AT THE POINT OF CONNECTION.
- CONTRACTOR TO CHLORINATE AND BACTERIA TEST.
- CLEAN OUT ALL EXISTING AND PROPOSED STORM INLETS AND CATCH BASINS AT THE COMPLETION OF CONSTRUCTION.
- SANITARY SEWER MAIN AND LATERALS SHALL BE SDR 26.
- CONTRACTOR SHALL COORDINATE WITH DRY UTILITY COMPANY'S REGARDING ANY POTENTIAL CONFLICTS AND COORDINATE RELOCATIONS AS MAY BE REQUIRED. CONTRACTOR SHALL ALSO COORDINATE THE PROPOSED INSTALLATION OF NEW FACILITIES AS REQUIRED.

- ALL WATER MAIN AND SERVICES SHALL BE INSTALLED AT A MINIMUM DEPTH OF 6.5' FROM TOP OF FINISHED GRADE ELEVATION TO TOP OF MAIN. PROVIDE 1.5' CLEAR SEPARATION IF WATER CROSSES BELOW SEWER AND MINIMUM 0.5' IF WATER CROSSES ABOVE.
- INSTALL 1 SHEET OF 4'x8'x4" HIGH DENSITY STYROFOAM INSULATION AT ALL LOCATIONS WHERE STORM SEWER CROSSES WATER MAIN OR WATER LATERALS.
- ALL SANITARY SEWER LATERALS SHALL BE CAPPED.
- REMOVE ALL WATER FLUSHING CONNECTIONS AT PLUG/CAP ONCE MAINS ARE FLUSHED.
- ALL NEW SEWERS SHALL HAVE A CLOSED CIRCUIT TELEVISION INSPECTION PERFORMED AFTER THE BINDER COURSE OF ASPHALT IS PLACED AND PRIOR TO ACCEPTANCE OF THESE SEWERS. THE TELEVISION REPORTS SHALL BE SUPPLIED TO THE CITY OF SUN PRAIRIE WASTE WATER TREATMENT PLANT FOR REVIEW. TELEVISION REPORTS MUST BE COMPATIBLE WITH WINCAN AND WITH IT PIPES. ANY DEFECTS AND/OR DEBRIS SHALL BE CORRECTED PRIOR TO THE ACCEPTANCE OF THESE SEWERS.
- PRIOR TO SANITARY SEWER INSTALLATION, A PLUG SHALL BE INSTALLED IN THE DOWNSTREAM MANHOLE INLET PIPE PER SPSS 503.8.2G.
- ALL STORM ENDWALLS LARGER THAN 12 INCHES IN EQUIVALENT DIAMETER MUST BE FURNISHED WITH DEBRIS GRATES AND TIE RODS.

DEMOLITION PLAN LEGEND

	CURB AND GUTTER REMOVAL
	ASPHALT REMOVAL
	CONCRETE REMOVAL
	BUILDING REMOVAL
	TREE REMOVAL
	SAWCUT
	UTILITY STRUCTURE REMOVAL
	UTILITY LINE REMOVAL

SITE PLAN LEGEND

	PROPERTY BOUNDARY
	CURB AND GUTTER (REVERSE CURB HATCHED)
	PROPOSED CHAIN LINK FENCE
	PROPOSED WOOD FENCE
	PROPOSED CONCRETE
	PROPOSED ASPHALT
	PROPOSED SIGN
	PROPOSED LIGHT POLE
	PROPOSED BOLLARD
	PROPOSED ADA DETECTABLE WARNING FIELD
	PROPOSED HANDICAP PARKING

GRADING LEGEND

	EXISTING MAJOR CONTOURS
	EXISTING MINOR CONTOURS
	PROPOSED MAJOR CONTOURS
	PROPOSED MINOR CONTOURS
	DITCH CENTERLINE
	SILT FENCE
	DISTURBED LIMITS
	BERM
	DRAINAGE DIRECTION
	PROPOSED SLOPE ARROWS
	EXISTING SPOT ELEVATIONS
	PROPOSED SPOT ELEVATIONS
	VELOCITY CHECK
	INLET PROTECTION
	EROSION MAT CLASS II, TYPE B
	TRACKING PAD
	RIP RAP

ABBREVIATIONS

TC – TOP OF CURB FF – FINISHED FLOOR FL – FLOW LINE SW – TOP OF WALK TW – TOP OF WALL BW – BOTTOM OF WALL	STMH – STORM MANHOLE FI – FIELD INLET CI – CURB INLET CB – CATCH BASIN EW – ENDWALL SMH – SANITARY MANHOLE
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PROPOSED UTILITY LEGEND

	STORM SEWER PIPE
	STORM SEWER MANHOLE
	STORM SEWER ENDWALL
	STORM SEWER CURB INLET
	STORM SEWER FIELD INLET
	ROOF DRAIN CLEANOUT
	SANITARY SEWER PIPE (GRAVITY)
	SANITARY SEWER LATERAL PIPE
	SANITARY SEWER MANHOLE
	SANITARY SEWER CLEANOUT
	WATER MAIN
	WATER SERVICE LATERAL PIPE
	FIRE HYDRANT
	WATER VALVE
	CURB STOP
	WATER VALVE MANHOLE
	PROPOSED PIPE INSULATION
	GAS MAIN
	ELECTRIC SERVICE

SURVEY LEGEND

	BENCHMARK
	PUBLIC LAND CORNER AS NOTED
	FOUND NAIL
	FOUND 1" Ø IRON PIPE
	FOUND P.K. NAIL
	FOUND 1 1/4" Ø IRON ROD
	FOUND 3/4" Ø IRON ROD
	FOUND RAILROAD SPIKE
	SET 3/4" x 18" SOLID IRON RE-ROD, WT. 1.50 lbs./ft. MIN.
	SET MAGNAIL
	RECORDED AS INFORMATION

LINEWORK LEGEND

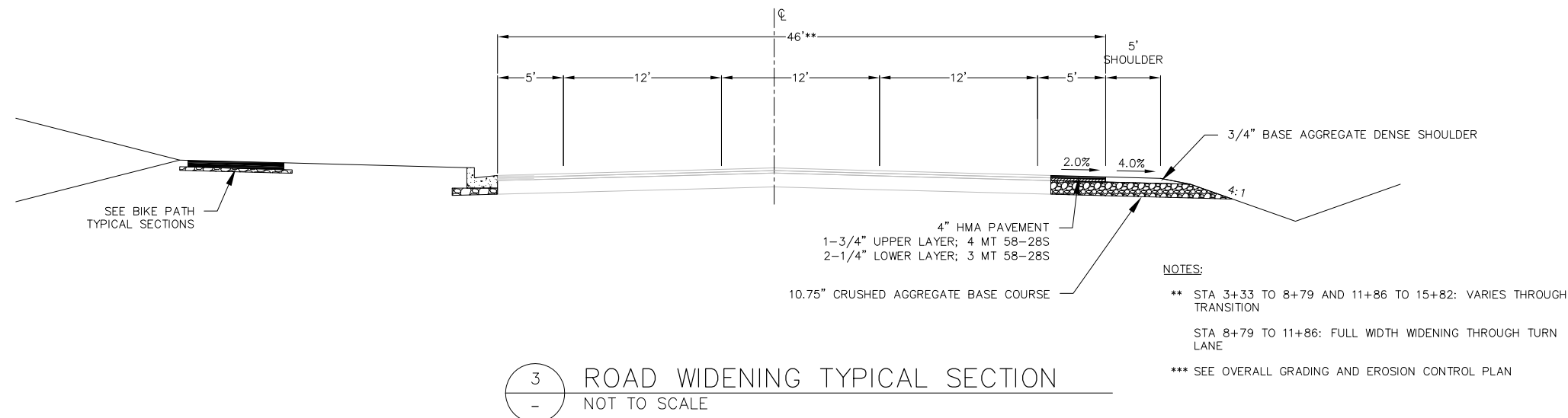
	EXISTING UNDERGROUND CABLE TV
	EXISTING UNDERGROUND TELEPHONE
	EXISTING RETAINING WALL
	EXISTING CHAIN LINK FENCE
	EXISTING WIRE FENCE
	EXISTING WOOD FENCE
	EXISTING GAS LINE
	EXISTING UNDERGROUND ELECTRIC LINE
	EXISTING OVERHEAD GENERAL UTILITIES
	EXISTING SANITARY SEWER LINE
	EXISTING STORM SEWER LINE
	EXISTING EDGE OF TREES
	EXISTING WATER MAIN
	EXISTING MAJOR CONTOUR
	EXISTING MINOR CONTOUR

SYMBOL LEGEND

	EXISTING BOLLARD
	EXISTING FLAG POLE
	EXISTING MAILBOX
	EXISTING POST
	EXISTING SIGN
	EXISTING CURB INLET
	EXISTING ENDWALL
	EXISTING FIELD INLET RECTANGULAR
	EXISTING FIELD INLET
	EXISTING STORM MANHOLE
	EXISTING SANITARY CLEANOUT
	EXISTING SANITARY MANHOLE
	EXISTING FIRE HYDRANT
	EXISTING FIRE DEPARTMENT CONNECTION
	EXISTING WATER MAIN VALVE
	EXISTING CURB STOP
	EXISTING WATER MANHOLE
	EXISTING GAS VALVE
	EXISTING GAS METER
	EXISTING AIR CONDITIONING PEDESTAL
	EXISTING DOWN GUY
	EXISTING ELECTRIC PEDESTAL
	EXISTING TRANSFORMER
	EXISTING ELECTRIC METER
	EXISTING LIGHT POLE
	EXISTING GENERIC LIGHT
	EXISTING UTILITY POLE
	EXISTING TV RECTANGULAR MANHOLE
	EXISTING TV PEDESTAL
	EXISTING TELEPHONE PEDESTAL
	EXISTING UNIDENTIFIED MANHOLE
	EXISTING UNIDENTIFIED UTILITY VAULT
	EXISTING HANDICAP PARKING
	EXISTING SHRUB
	EXISTING CONIFEROUS TREE
	EXISTING DECIDUOUS TREE

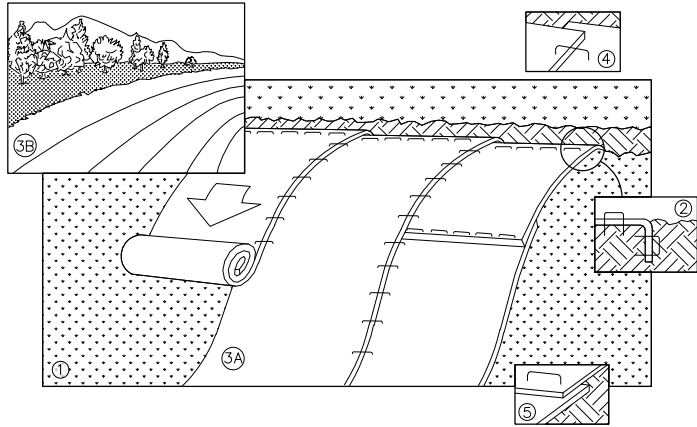
HATCHING LEGEND

	EXISTING WARNING PAD
	EXISTING CONCRETE PAVEMENT/SIDEWALK
	EXISTING ASPHALT
	EXISTING WASHED STONE OR SAND OR RUBBER MAT



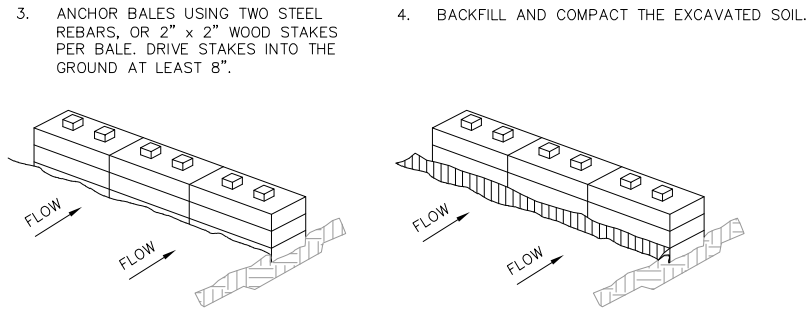
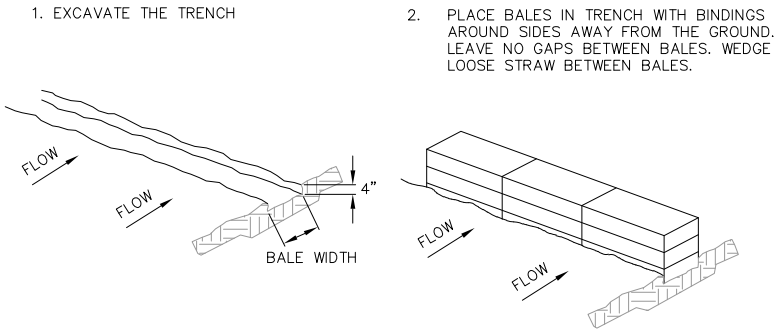
EROSION CONTROL MEASURES

- EROSION CONTROL SHALL BE IN ACCORDANCE WITH THE CITY OF SUN PRAIRIE EROSION CONTROL ORDINANCE AND CHAPTER NR 216 OF THE WISCONSIN ADMINISTRATIVE CODE.
- CONSTRUCT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH WISCONSIN DNR TECHNICAL STANDARDS (<http://dnr.wi.gov/runoff/stormwater/techstds.htm>) AND WISCONSIN CONSTRUCTION SITE BEST MANAGEMENT PRACTICE HANDBOOK.
- INSTALL SEDIMENT CONTROL PRACTICES (TRACKING PAD, PERIMETER SILT FENCE, SEDIMENT BASINS, ETC.) PRIOR TO INITIATING OTHER LAND DISTURBING CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR IS REQUIRED TO MAKE EROSION CONTROL INSPECTIONS AT THE END OF EACH WEEK AND WHEN 0.5 INCHES OF RAIN FALLS WITHIN 24 HOURS. INSPECTION REPORTS SHALL BE PREPARED AND FILED AS REQUIRED BY THE DNR AND/OR CITY. ALL MAINTENANCE WILL FOLLOW AN INSPECTION WITHIN 24 HOURS.
- EROSION CONTROL IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ACCEPTANCE OF THIS PROJECT. EROSION CONTROL MEASURES AS SHOWN SHALL BE THE MINIMUM PRECAUTIONS THAT WILL BE ALLOWED. ADDITIONAL EROSION CONTROL MEASURES, AS REQUESTED IN WRITING BY THE STATE OR LOCAL INSPECTORS, OR THE DEVELOPER'S ENGINEER, SHALL BE INSTALLED WITHIN 24 HOURS.
- A 3" CLEAR STONE TRACKING PAD SHALL BE INSTALLED AT THE END OF CONSTRUCTION LIMITS TO PREVENT SEDIMENT FROM BEING TRACKED ONTO THE ADJACENT PAVED PUBLIC ROADWAY. SEDIMENT TRACKING PAD SHALL CONFORM TO WisDNR TECHNICAL STANDARD 1057. SEDIMENT REACHING THE PUBLIC ROAD SHALL BE REMOVED BY STREET CLEANING (NOT HYDRAULIC FLUSHING) BEFORE THE END OF EACH WORK DAY.
- CHANNELIZED RUNOFF:** FROM ADJACENT AREAS PASSING THROUGH THE SITE SHALL BE DIVERTED AROUND DISTURBED AREAS.
- STABILIZED DISTURBED GROUND:** ANY SOIL OR DIRT PILES WHICH WILL REMAIN IN EXISTENCE FOR MORE THAN 7-CONSECUTIVE DAYS, WHETHER TO BE WORKED DURING THAT PERIOD OR NOT, SHALL NOT BE LOCATED WITHIN 25- FEET OF ANY ROADWAY, PARKING LOT, PAVED AREA, OR DRAINAGE STRUCTURE OR CHANNEL (UNLESS INTENDED TO BE USED AS PART OF THE EROSION CONTROL MEASURES). TEMPORARY STABILIZATION AND CONTROL MEASURES (SEEDING, MULCHING, TARPING, EROSION MATTING, BARRIER FENCING, ETC.) ARE REQUIRED FOR THE PROTECTION OF DISTURBED AREAS AND SOIL PILES, WHICH WILL REMAIN UN-WORKED FOR A PERIOD OF MORE THAN 7-CONSECUTIVE CALENDAR DAYS. THESE MEASURES SHALL REMAIN IN PLACE UNTIL SITE HAS STABILIZED.
- SITE DE-WATERING:** WATER PUMPED FROM THE SITE SHALL BE TREATED BY TEMPORARY SEDIMENTATION BASINS OR OTHER APPROPRIATE CONTROL MEASURES. SEDIMENTATION BASINS SHALL HAVE A DEPTH OF AT LEAST 3 FEET, BE SURROUNDED BY SNOWFENCE OR EQUIVALENT BARRIER AND HAVE SUFFICIENT SURFACE AREA TO PROVIDE A SURFACE SETTLING RATE OF NO MORE THAN 750 GALLONS PER SQUARE FOOT PER DAY AT THE HIGHEST DEWATERING PUMPING RATE. WATER MAY NOT BE DISCHARGED IN A MANNER THAT CAUSES EROSION OF THE SITE, A NEIGHBORING SITE, OR THE BED OR BANKS OF THE RECEIVING WATER. POLYMERS MAY BE USED AS DIRECTED BY DNR TECHNICAL STANDARD 1061 (DE-WATERING).
- WASHED STONE WEEPERS OR TEMPORARY EARTH BERMS SHALL BE BUILT PER PLAN BY CONTRACTOR TO TRAP SEDIMENT OR SLOW THE VELOCITY OF STORM WATER.
- SEE DETAIL SHEETS FOR RIP-RAP SIZING. IN NO CASE WILL RIP-RAP BE SMALLER THAN 3" TO 6".
- INLET FILTERS ARE TO BE PLACED IN STORMWATER INLET STRUCTURES AS SOON AS THEY ARE INSTALLED. ALL PROJECT AREA STORM INLETS NEED WISCONSIN D.O.T. TYPE D INLET PROTECTION. THE FILTERS SHALL BE MAINTAINED UNTIL THE CITY HAS ACCEPTED THE BINDER COURSE OF ASPHALT.
- RESTORATION (SEED, FERTILIZE AND MULCH) SHALL BE PER SPECIFICATIONS ON THIS SHEET (NOTE: ADD SEEDING RATE STANDARD OF DETAIL BLOCK TO PLAN) UNLESS SPECIAL RESTORATION IS CALLED FOR ON THE LANDSCAPE PLAN OR THE DETENTION BASIN DETAIL SHEET.
- TERRACES SHALL BE RESTORED WITH 6" TOPSOIL, PERMANENT SEED, FERTILIZER AND MULCH. LOTS SHALL BE RESTORED WITH 6" TOPSOIL, TEMPORARY SEED, FERTILIZER AND MULCH.
- SEED, FERTILIZER AND MULCH SHALL BE APPLIED WITHIN 7 DAYS AFTER FINAL GRADE HAS BEEN ESTABLISHED. IF DISTURBED AREAS WILL NOT BE RESTORED IMMEDIATELY AFTER ROUGH GRADING, TEMPORARY SEED SHALL BE PLACED.
- FOR THE FIRST SIX WEEKS AFTER RESTORATION (E.G. SEED & MULCH, EROSION MAT, SOD) OF A DISTURBED AREA, INCLUDE SUMMER WATERING PROVISIONS OF ALL NEWLY SEEDED AND MULCHED AREAS WHENEVER 7 DAYS ELAPSE WITHOUT A RAIN EVENT.
- EROSION MAT (CLASS I, TYPE B URBAN PER WISCONSIN D.O.T. P.A.L.) SHALL BE INSTALLED ON THE BOTTOM (INVERT) OF ROADSIDE DITCHES/SWALES AS SHOWN ON THIS PLAN, 1 ROLL WIDTH.
- SOIL STABILIZERS SHALL BE APPLIED TO DISTURBED AREAS WITH SLOPES BETWEEN 10% AND 3:1 (DO NOT USE IN CHANNELS). SOIL STABILIZERS SHALL BE TYPE B, PER WISCONSIN D.O.T. P.A.L. (PRODUCT ACCEPTABILITY LIST), OR EQUAL. APPLY AT RATES AND METHODS SPECIFIED PER MANUFACTURER. SOIL STABILIZERS SHALL BE RE-APPLIED WHENEVER VEHICLES OR OTHER EQUIPMENT TRACK ON THE AREA.
- SILT FENCE OR EROSION MAT SHALL BE INSTALLED ALONG THE CONTOURS AT 100 FOOT INTERVALS DOWN THE SLOPE ON THE DISTURBED SLOPES STEEPER THAN 5% AND MORE THAN 100 FEET LONG THAT SHEET FLOW TO THE ROADWAY UNLESS SOIL STABILIZERS ARE USED.
- SILT FENCE TO BE USED ACROSS AREAS OF THE LOT THAT SLOPE TOWARDS A PUBLIC STREET OR WATERWAY. SEE DETAILS.
- SEDIMENT SHALL BE CLEANED FROM CURB AND GUTTER AFTER EACH RAINFALL AND PRIOR TO PROJECT ACCEPTANCE.
- ACCUMULATED CONSTRUCTION SEDIMENT SHALL BE REMOVED FROM ALL PERMANENT BASINS TO THE ELEVATION SHOWN ON THE GRADING PLAN FOLLOWING THE STABILIZATION OF DRAINAGE AREAS.
- ALL CONSTRUCTION ENTRANCES SHALL HAVE TEMPORARY ROAD CLOSED SIGNS THAT WILL BE IN PLACE WHEN THE ENTRANCE IS NOT IN USE AND AT THE END OF EACH DAY.
- ANY PROPOSED CHANGES TO THE EROSION CONTROL PLAN MUST BE SUBMITTED AND APPROVED BY DANE COUNTY LAND CONSERVATION OR PERMITTING MUNICIPALITY.
- THE CITY, OWNER AND/OR ENGINEER MAY REQUIRE ADDITIONAL EROSION CONTROL MEASURES AT ANY TIME DURING CONSTRUCTION.

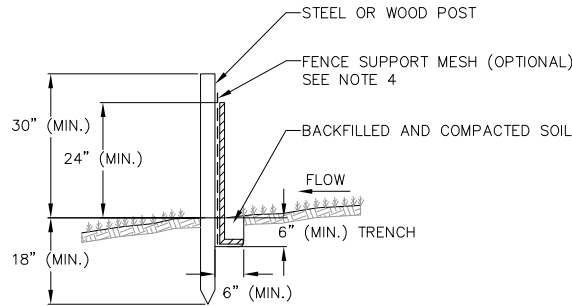


- NOTE: REFER TO GENERAL STAPLE PATTERN GUIDE FOR CORRECT STAPLE PATTERN RECOMMENDATIONS FOR SLOPE INSTALLATIONS.
- PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING APPLICATION OF FERTILIZER AND SEED. NOTE: WHEN USING CELL-O-SEED, DO NOT SEED PREPARED AREA. CELL-O-SEED MUST BE INSTALLED WITH PAPER SIDE DOWN.
 - BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN 6" DEEP BY 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.
 - ROLL THE BLANKETS <A> DOWN, OR HORIZONTALLY ACROSS THE SLOPE.
 - THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH APPROXIMATELY 2" OVERLAP.
 - WHEN BLANKETS MUST BE SPLICED DOWN THE SLOPE, PLACE BLANKETS END OVER END (SHINGLE STYLE) WITH APPROXIMATELY 4" OVERLAP. STAPLE THROUGH OVERLAPPED AREA, APPROXIMATELY 12" APART.
 - ALL BLANKETS MUST BE SECURELY FASTENED TO THE SLOPE BY PLACING STAPLES/STAKES IN APPROPRIATE LOCATIONS AS RECOMMENDED BY THE MANUFACTURER.

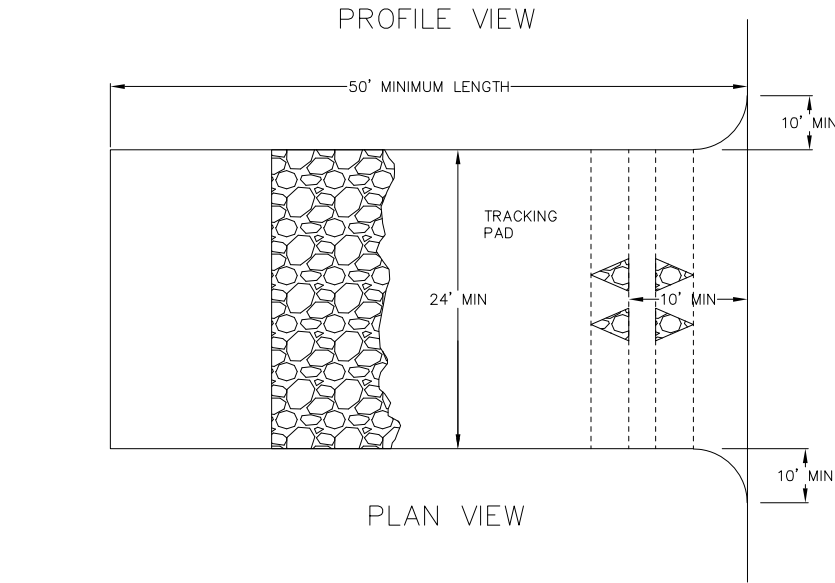
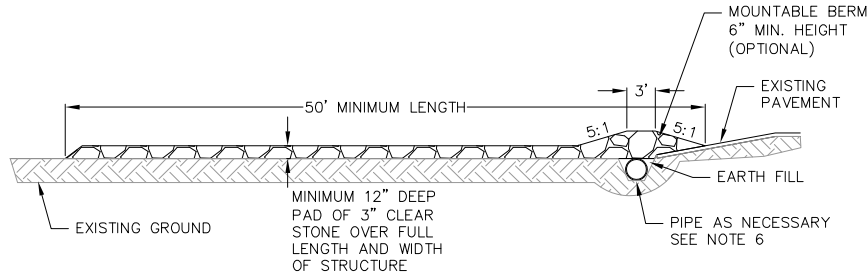
1 EROSION MAT
NOT TO SCALE



2 STRAW BALE DITCH CHECK
NOT TO SCALE



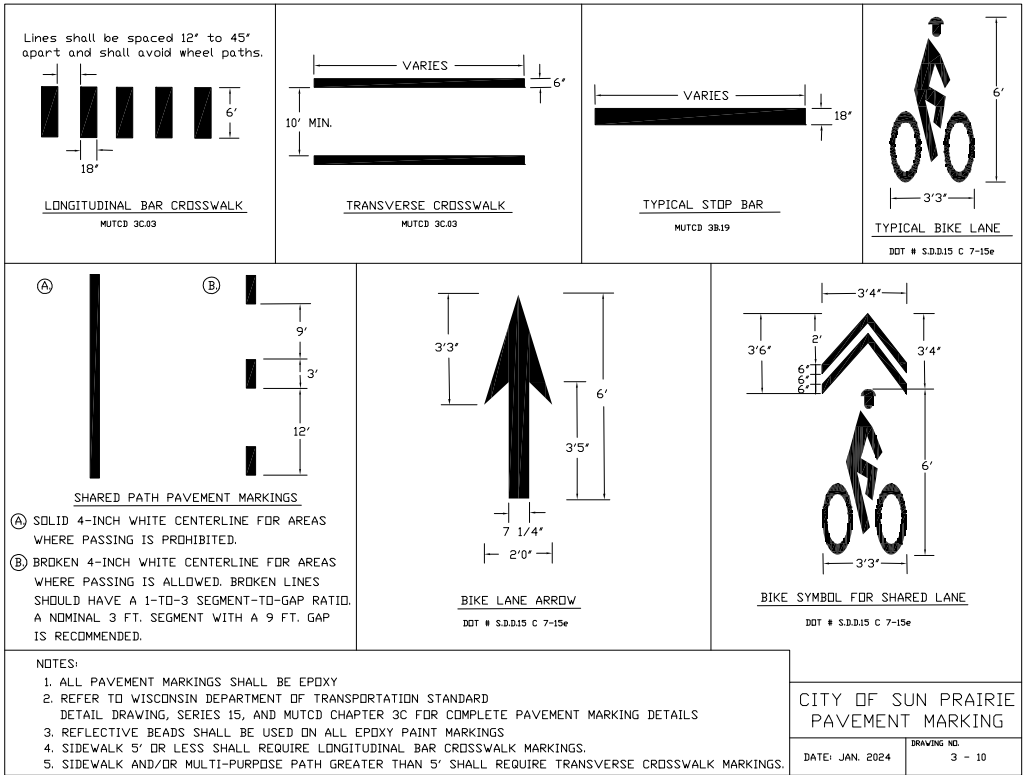
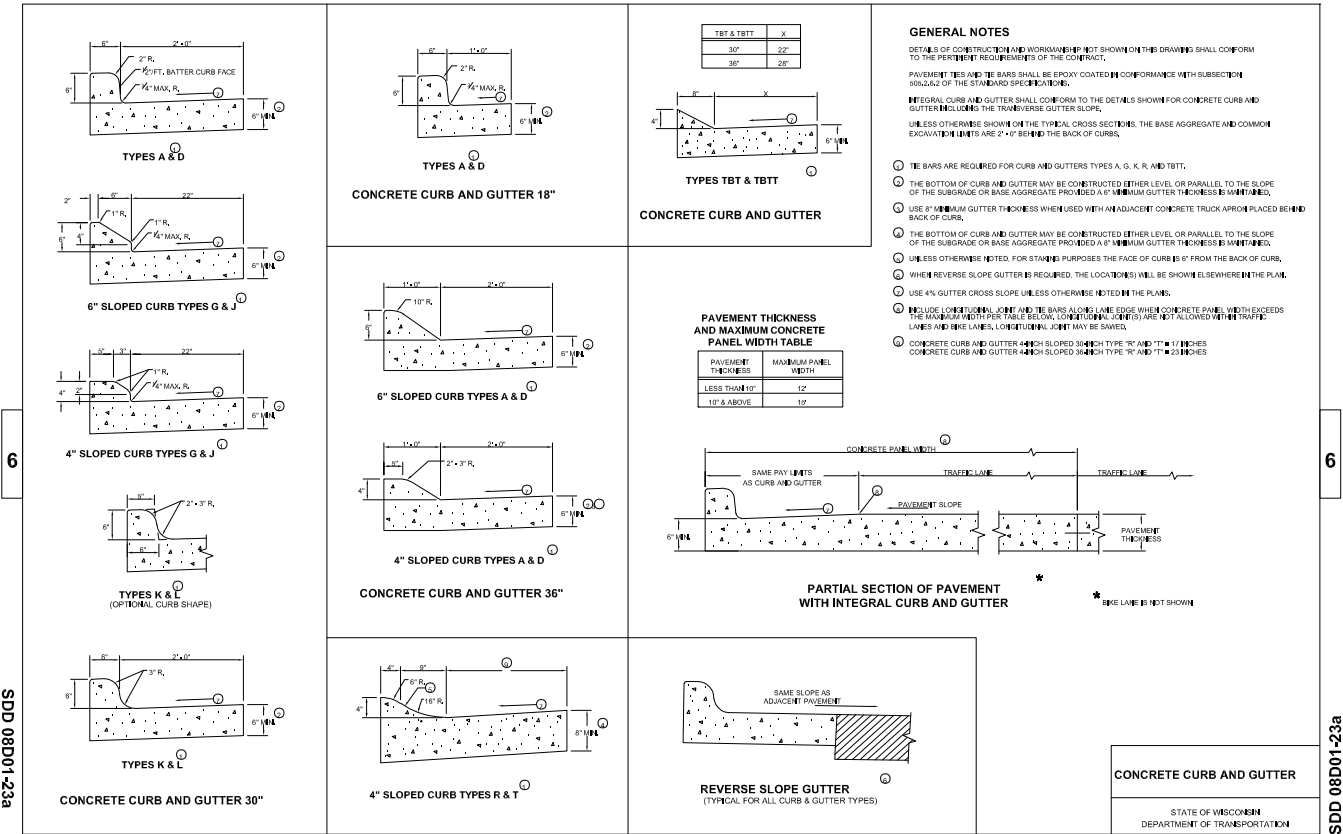
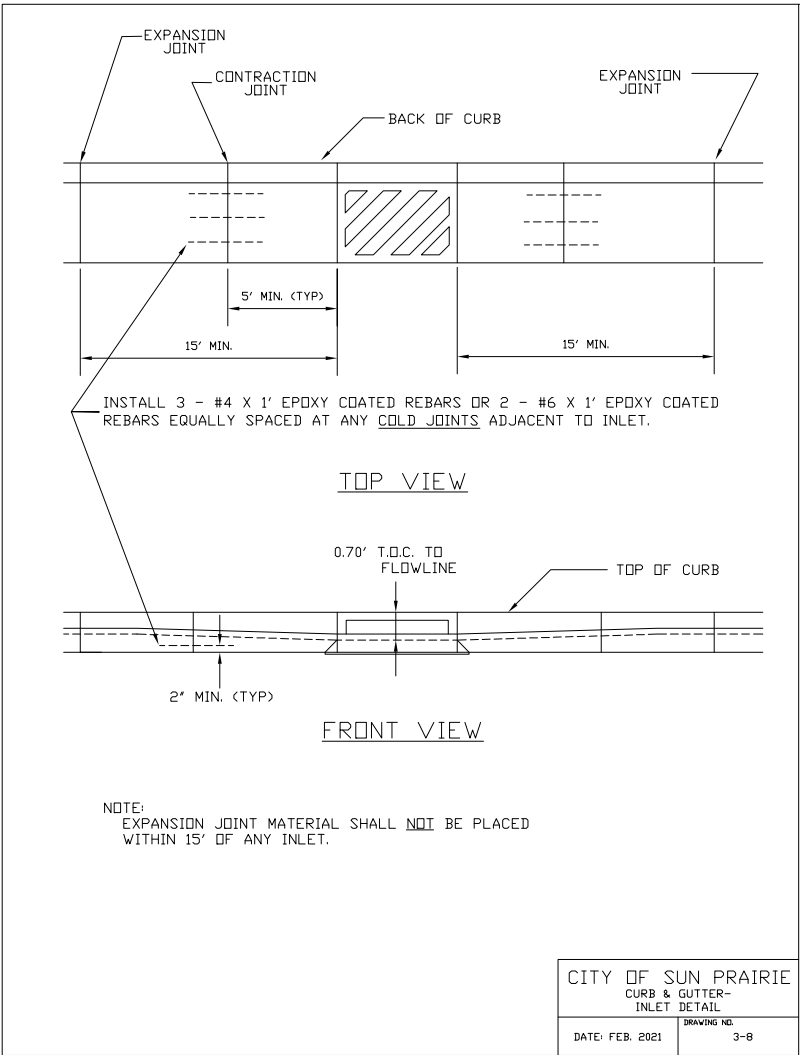
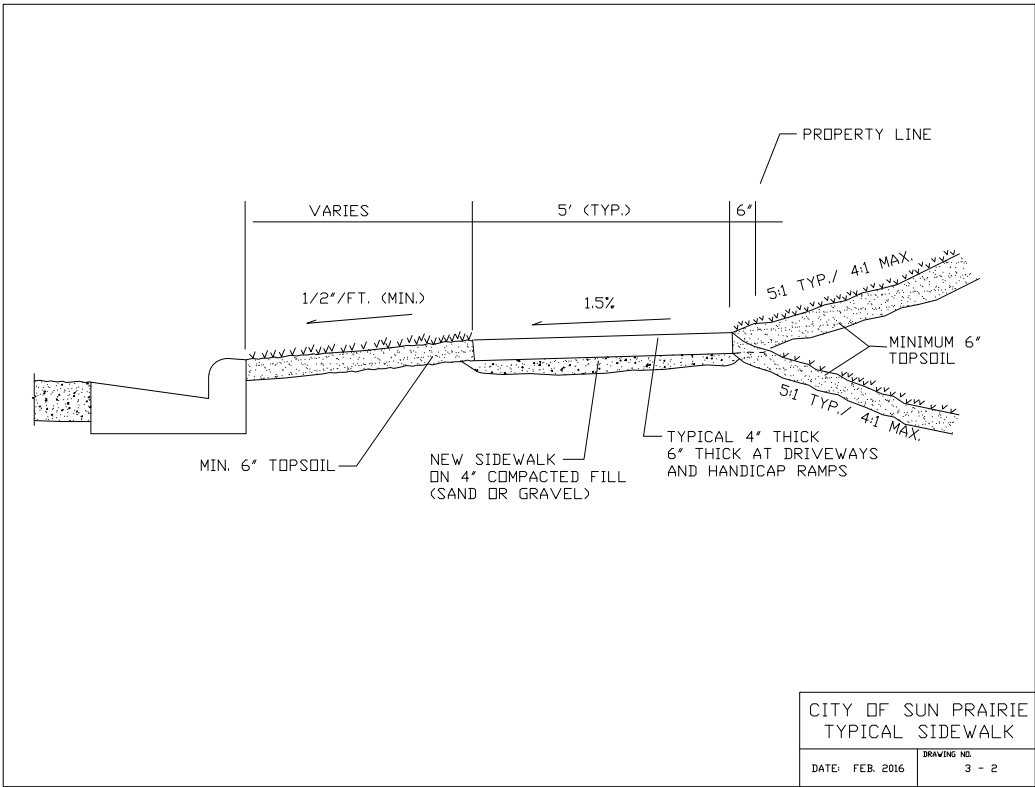
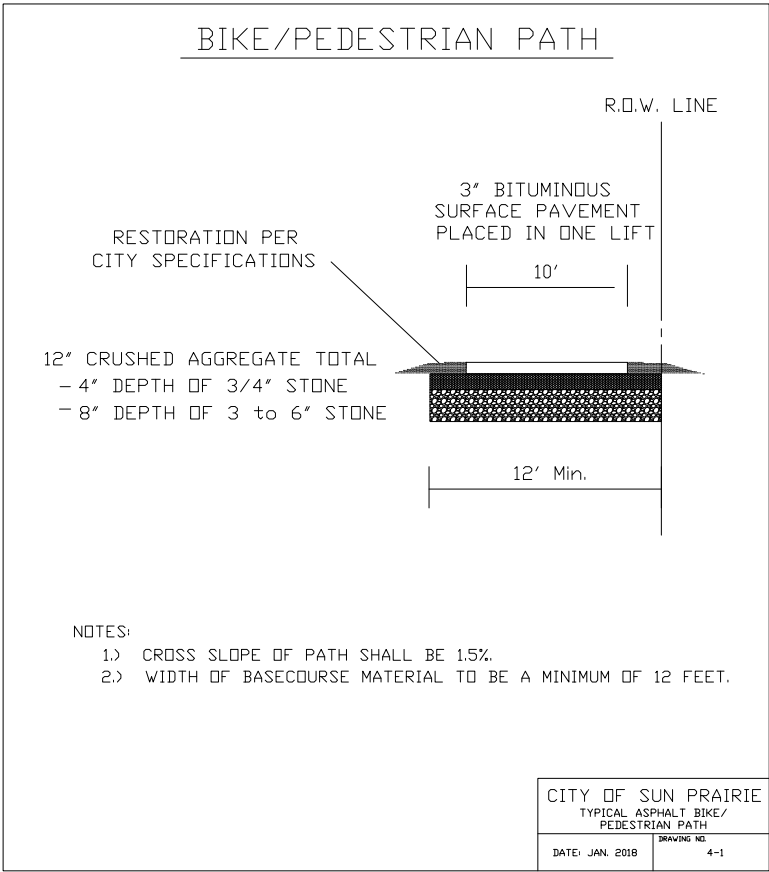
3 SILT FENCE
NOT TO SCALE

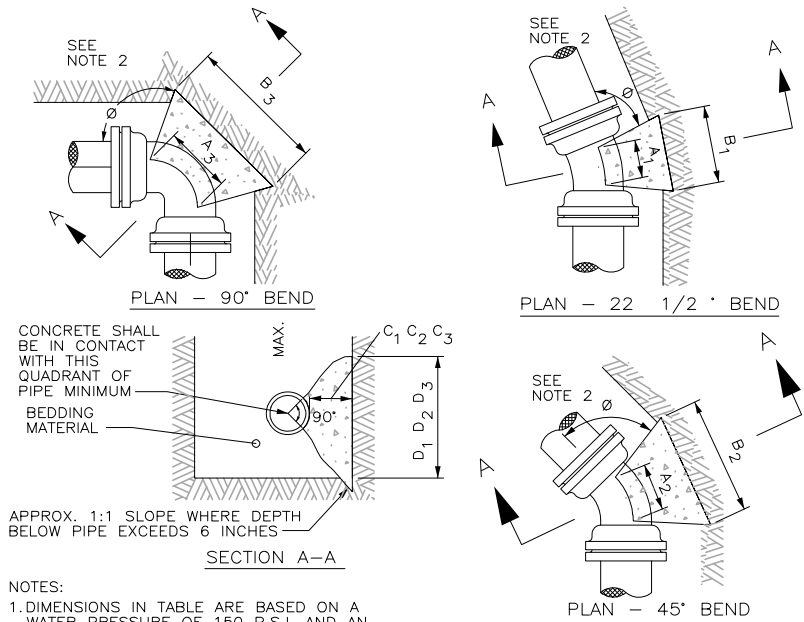


- FOLLOW WISCONSIN DNR TECHNICAL STANDARD 1057 FOR FURTHER DETAILS AND INSTALLATION.
- LENGTH - MINIMUM OF 50'
- WIDTH - 24' MINIMUM, SHOULD BE FLARED AT THE EXISTING ROAD TO PROVIDE A TURNING RADIUS.
- ON SITES WITH A HIGH GROUND WATER TABLE OR WHERE SATURATED CONDITIONS EXIST, GEOTEXTILE FABRIC SHALL BE PLACED OVER EXISTING GROUND PRIOR TO PLACING STONE. FABRIC SHALL BE WISDOT TYPE-HR GEOTEXTILE FABRIC.
- STONE - CRUSHED 3" CLEAR STONE SHALL BE PLACED AT LEAST 12" DEEP OVER THE ENTIRE LENGTH AND WIDTH OF ENTRANCE.
- SURFACE WATER - ALL SURFACE WATER FLOWING TO OR DIVERTED TOWARDS CONSTRUCTION ENTRANCES SHALL BE PIPED THROUGH THE ENTRANCE. MAINTAINING POSITIVE DRAINAGE. PIPE INSTALLED THROUGH THE STABILIZED CONSTRUCTION ENTRANCE SHALL BE PROTECTED WITH A MOUNTABLE BERM WITH 5:1 SLOPES AND MINIMUM OF 6" STONE OVER THE PIPE. PIPE SHALL BE SIZED ACCORDING TO THE DRAINAGE REQUIREMENTS. WHEN THE ENTRANCE IS LOCATED AT A HIGH SPOT AND HAS NO DRAINAGE TO CONVEY A PIPE SHALL NOT BE NECESSARY. THE MINIMUM PIPE DIAMETER SHALL BE 6". CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF SAID PIPE.
- LOCATION - A STABILIZED CONSTRUCTION ENTRANCE SHALL BE LOCATED WHERE CONSTRUCTION TRAFFIC ENTERS AND/OR LEAVES THE CONSTRUCTION SITE. VEHICLES LEAVING THE SITE MUST TRAVEL OVER THE ENTIRE LENGTH OF THE TRACKING PAD.

4 TRACKING PAD
NOT TO SCALE

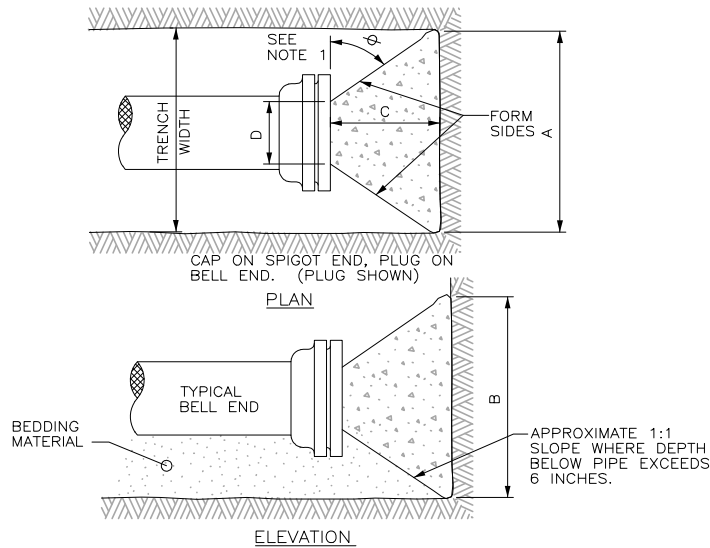
- NOTES:
- INSTALL SILT FENCE TO FOLLOW THE GROUND CONTOURS AS CLOSELY AS POSSIBLE.
 - CURVE THE SILT FENCE UP THE SLOPE TO PREVENT WATER FROM RUNNING AROUND THE ENDS.
 - POST SPACING WITH FENCE SUPPORT MESH = 10 FT. (MAX.)
POST SPACING WITHOUT FENCE SUPPORT MESH = 6 FT. (MAX.)
 - SILT FENCE SUPPORT MESH CONSISTS OF 14-GAUGE STEEL WIRE WITH A MESH SPACING OF 6 IN. X 6 IN. OR PREFABRICATED POLYMERIC MESH OF EQUIVALENT STRENGTH





PIPE SIZE	BUTTRESS DIMENSIONS					
	22 1/2° BENDS	45° BENDS	90° BENDS			
	B ₁	D ₁	B ₂	D ₂	B ₃	D ₃
6"	1'-0"	1'-0"	1'-0"	1'-0"	1'-4"	1'-2"
8"	1'-0"	1'-0"	1'-4"	1'-2"	1'-10"	1'-6"
10"	1'-2"	1'-2"	1'-7"	1'-7"	2'-3"	1'-10"
12"	1'-4"	1'-4"	1'-10"	1'-10"	2'-8"	2'-3"
16"	1'-10"	1'-8"	2'-6"	2'-4"	3'-10"	2'-10"
20"	2'-4"	2'-0"	3'-3"	2'-10"	5'-0"	3'-4"
24"	2'-10"	2'-4"	4'-0"	3'-3"	6'-4"	3'-10"

BUTRESS FOR BENDS DETAIL
SCALE: NONE

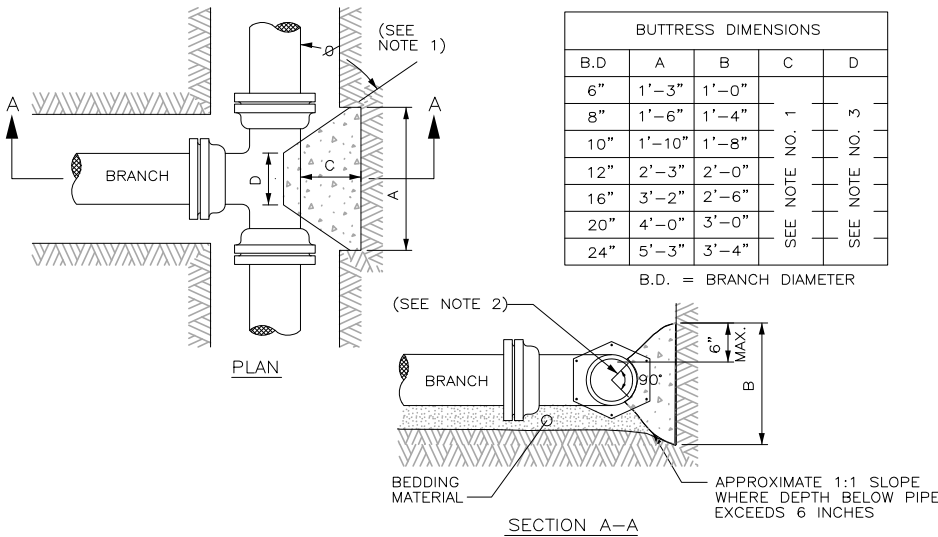


DIA.	BUTTRESS DIMENSIONS			
	A	B	C	D
6"	1'-6"	1'-2"	1	2
8"	2'-0"	1'-4"		
12"	2'-5"	1'-10"		
16"	3'-4"	2'-4"		
20"	4'-3"	2'-10"		
24"	5'-2"	3'-4"		
30"	6'-9"	4'-0"		

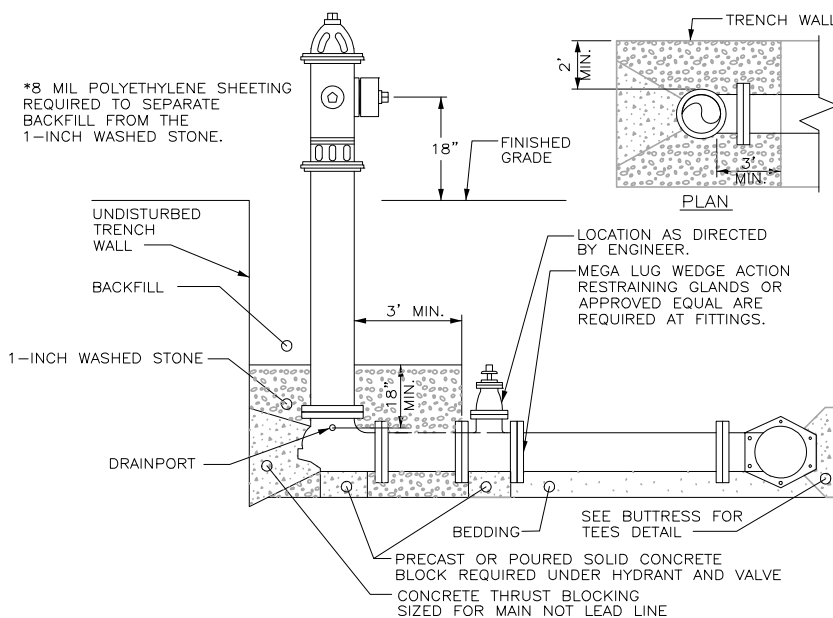
BUTRESS FOR PLUGS DETAIL
SCALE: NONE

CITY OF SUN PRAIRIE
BUTTRESS FOR BENDS AND PLUGS

DATE: FEB. 2011 DRAWING NO. 6-1



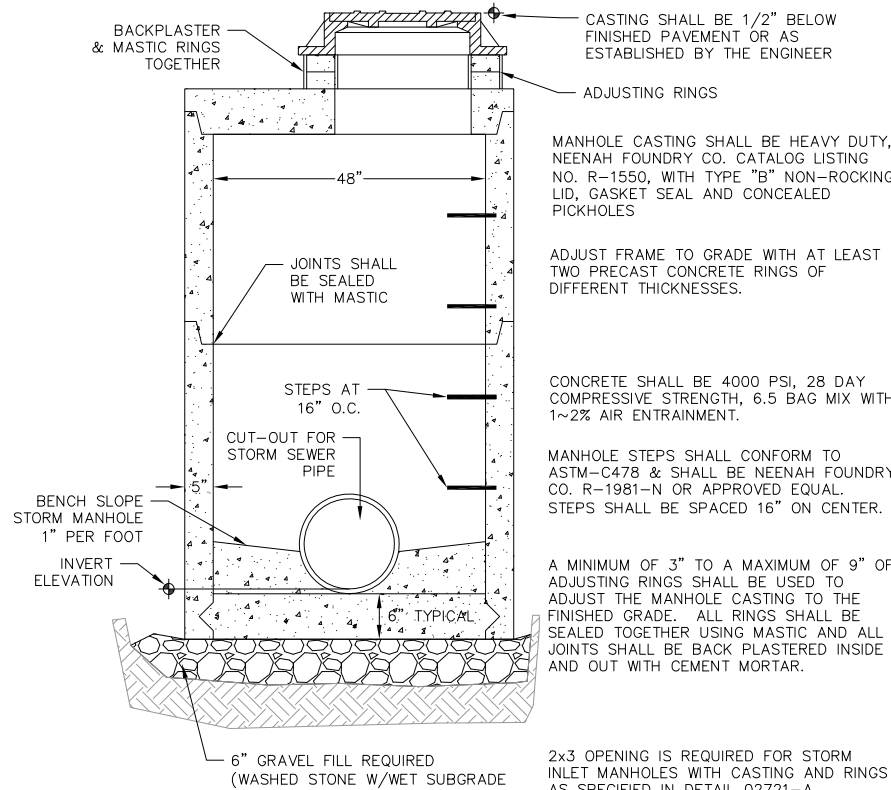
BUTRESS FOR TEES DETAIL
SCALE: NONE



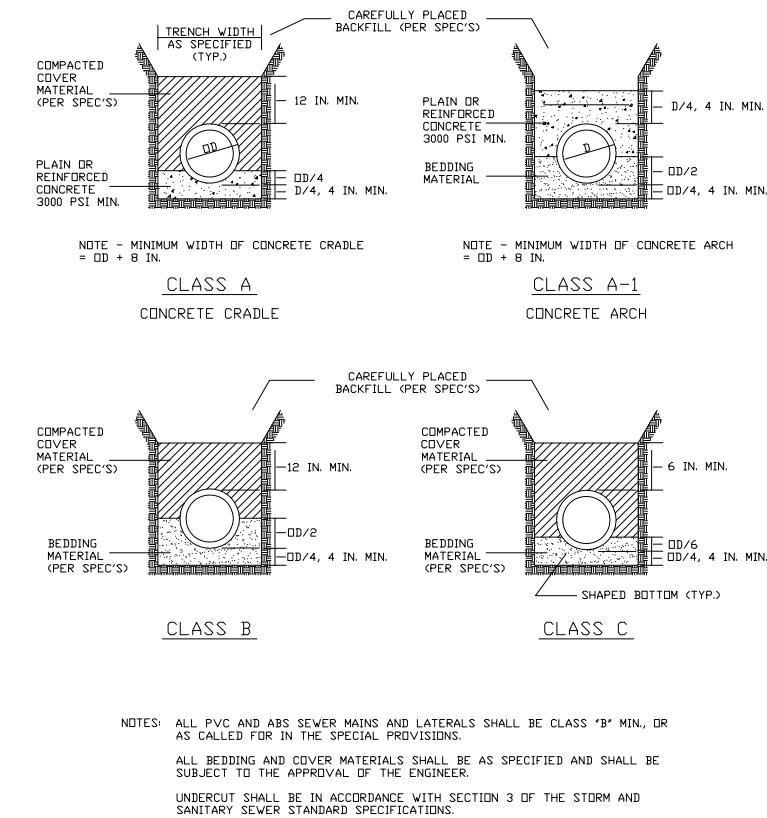
HYDRANT DETAIL
SCALE: NONE

CITY OF SUN PRAIRIE
BUTTRESS FOR TEES/HYDRANT DETAIL

DATE: FEB. 2023 DRAWING NO. 6-2



1 48" STORM SEWER MANHOLE
NOT TO SCALE



CITY OF SUN PRAIRIE
STORM AND SANITARY SEWER
BEDDING STANDARDS

DATE: SEPT. 1996 DRAWING NO. 5-1

NO.	DATE	REVISIONS	NO.	DATE	REMARKS

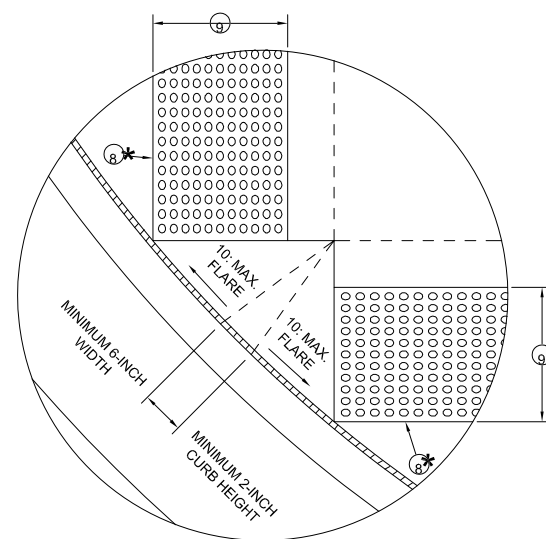
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11/5/2024

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ZDRE

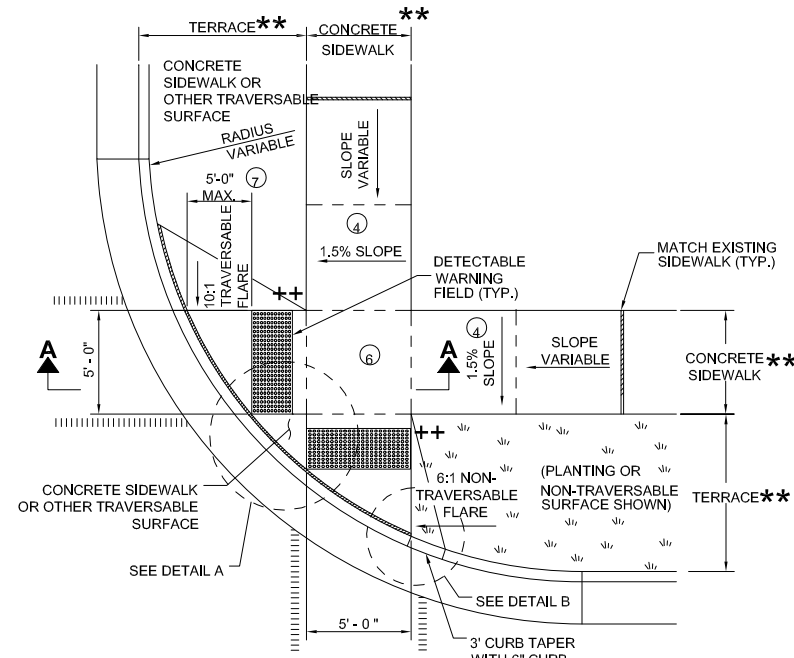
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JZAM

PROJECT NO.
210178

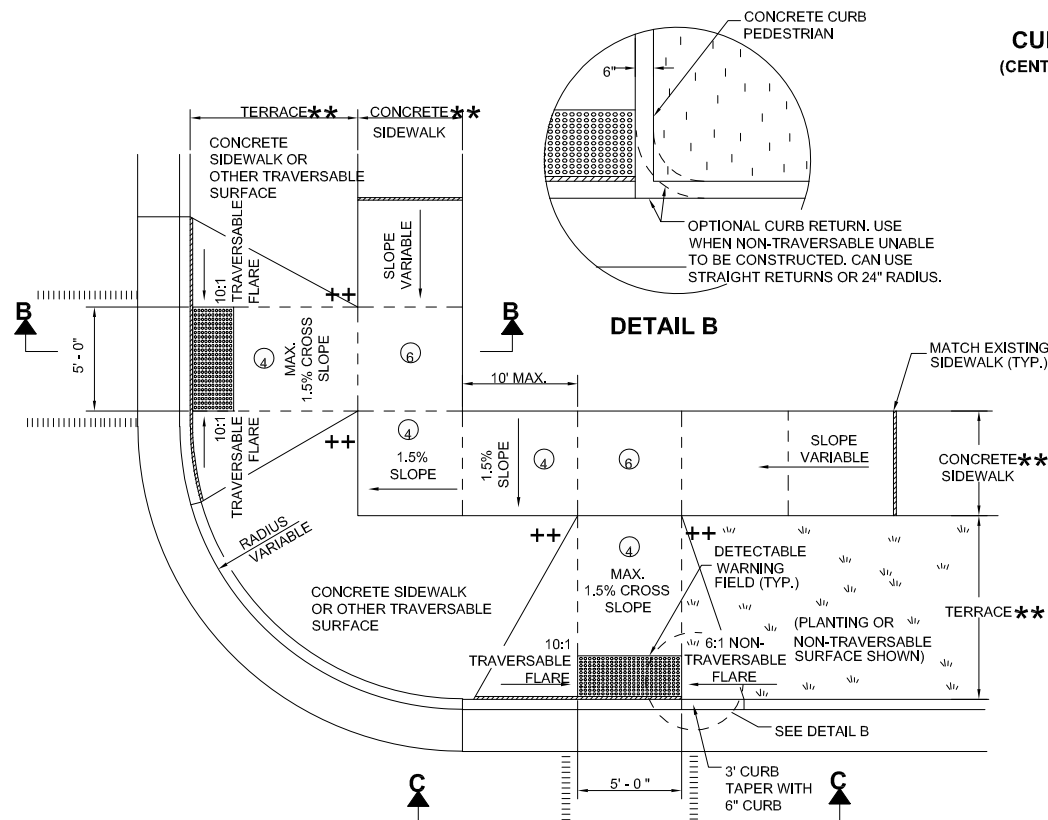
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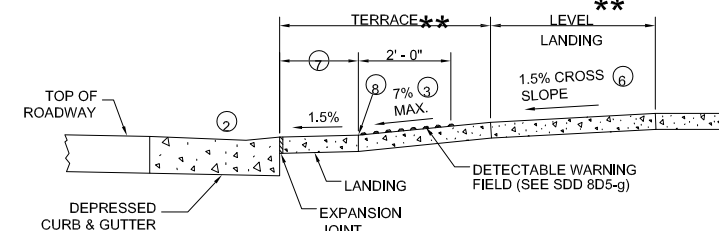
DETAIL A



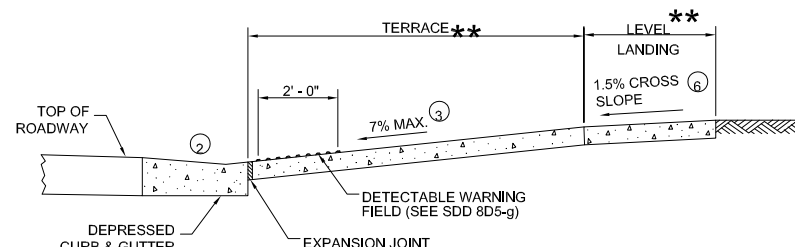
PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)



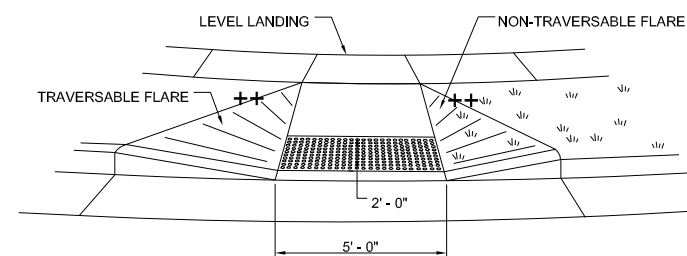
PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.

- ④ GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE. MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 805-F.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ON THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

* MAXIMUM 2.0% SLOPE
IN ALL DIRECTIONS IN
FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE
IN THE PLANS

++ CONSTRUCT 6" WEDGE TO
AVOID CONCRETE BREAKAGE

LEGEND

	1/2" EXPANSION JOINT SIDEWALK
	CONTRACTION JOINT SIDEWALK
	PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	REMARKS	NO.	DATE	REMARKS

	2		
DATE			
11/5/2024			

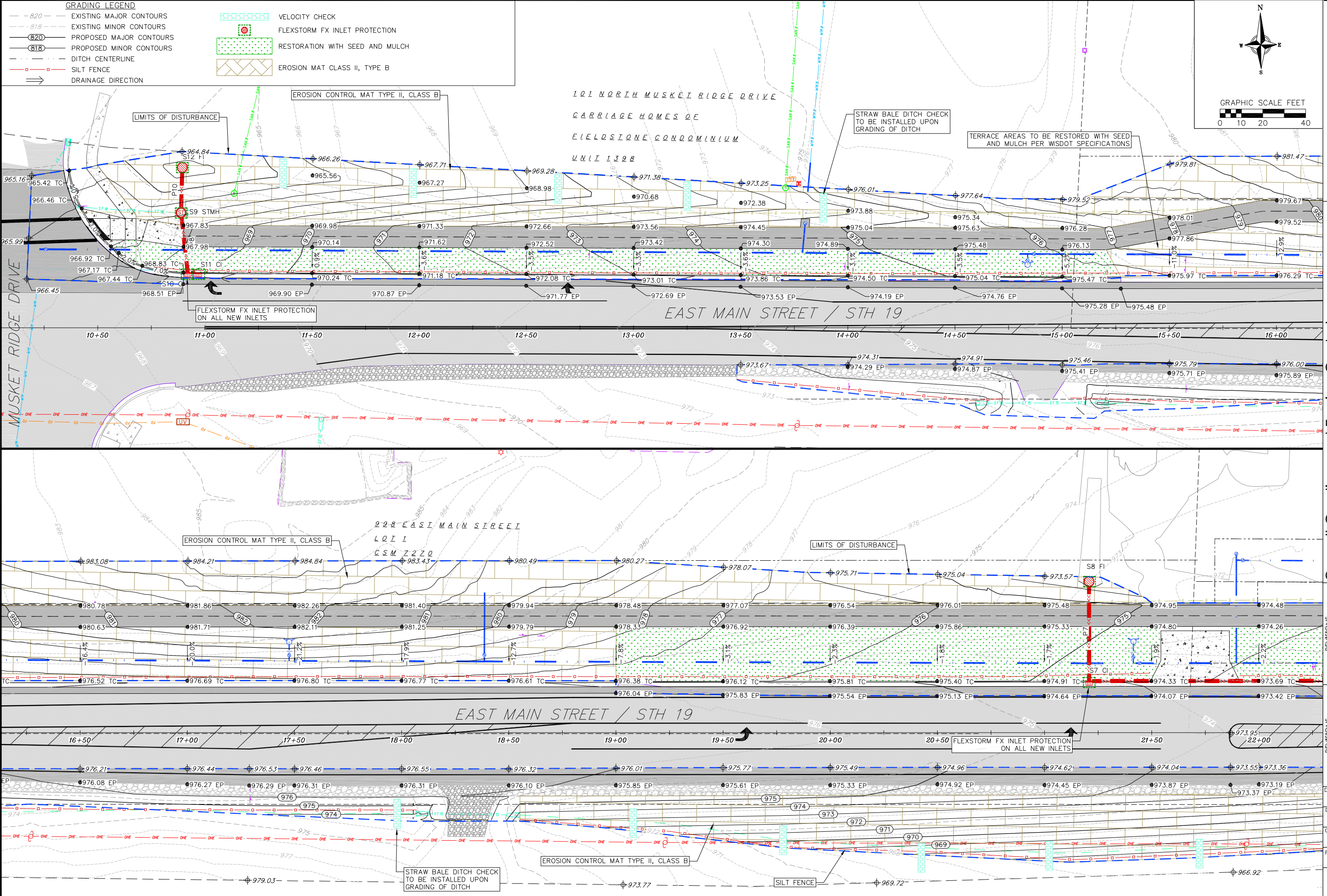
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JZAM

PROJECT NO.	210178
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C3.4

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owners engineers advisors

Overall Grading and Erosion Control
Bike Path and Public Utility Improvements
Sun Prairie
Dane County, WI

REVISIONS	
NO.	DATE

REVISIONS	
NO.	DATE

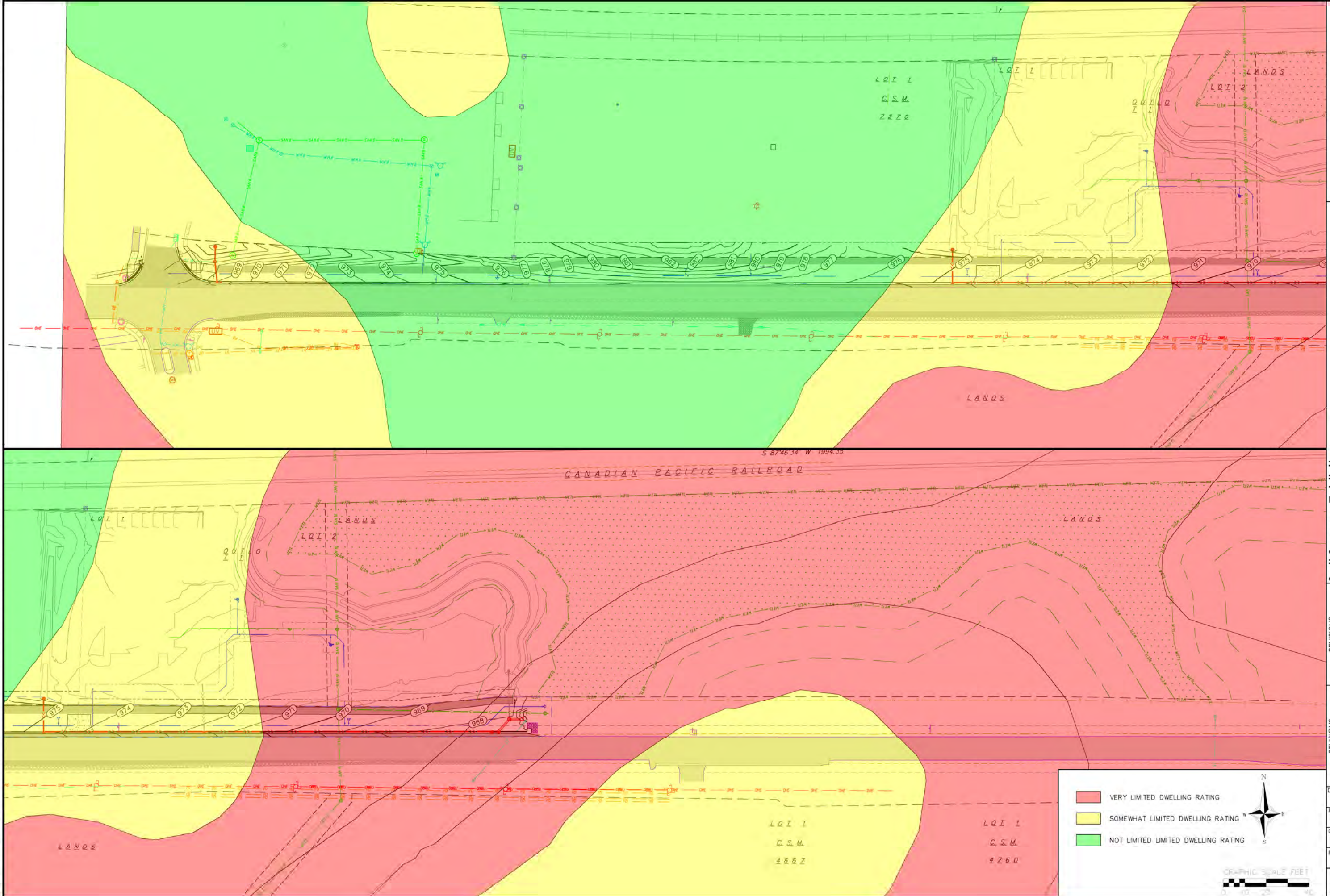
DATE
11/05/2024

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JZAM

PROJECT NO.
210178

C4.0



VERY LIMITED DWELLING RATING

SOMEWHAT LIMITED DWELLING RATING

NOT LIMITED LIMITED DWELLING RATING


N

S

E

W

GRAPHIC SCALE FEET
0 10 20 40



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Soil Survey Exhibit

Bike Path and Public Utility Improvements

Sun Prairie

Dane County, WI

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

DATE

11/05/2024

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ZDRE

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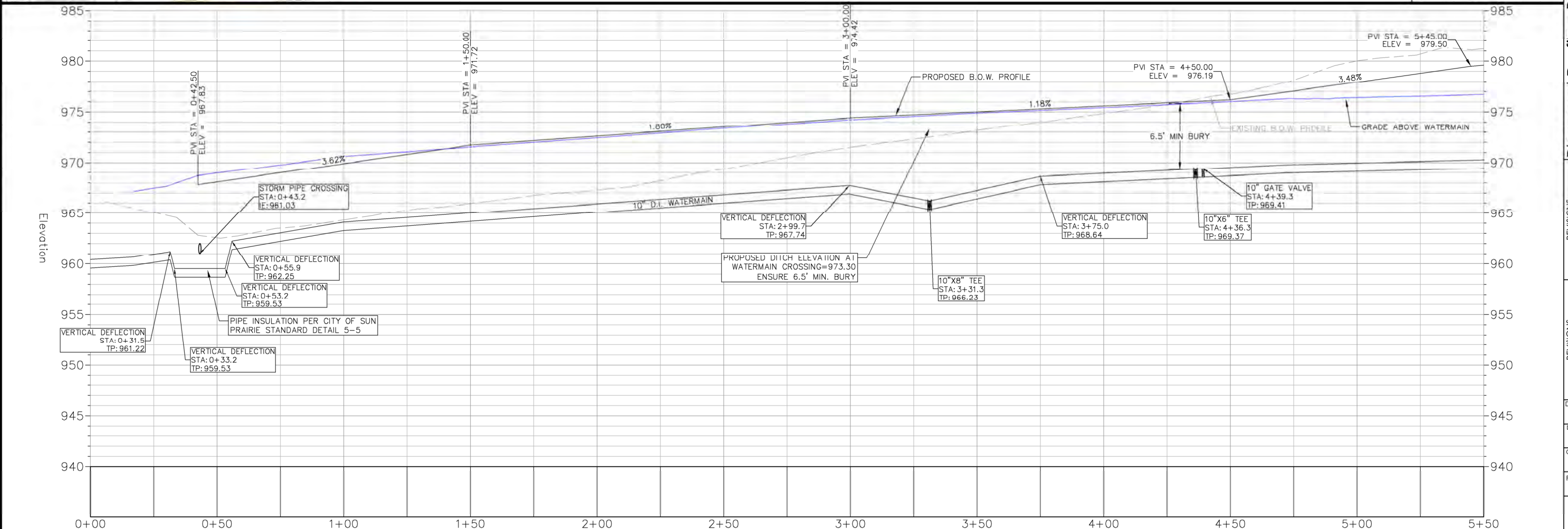
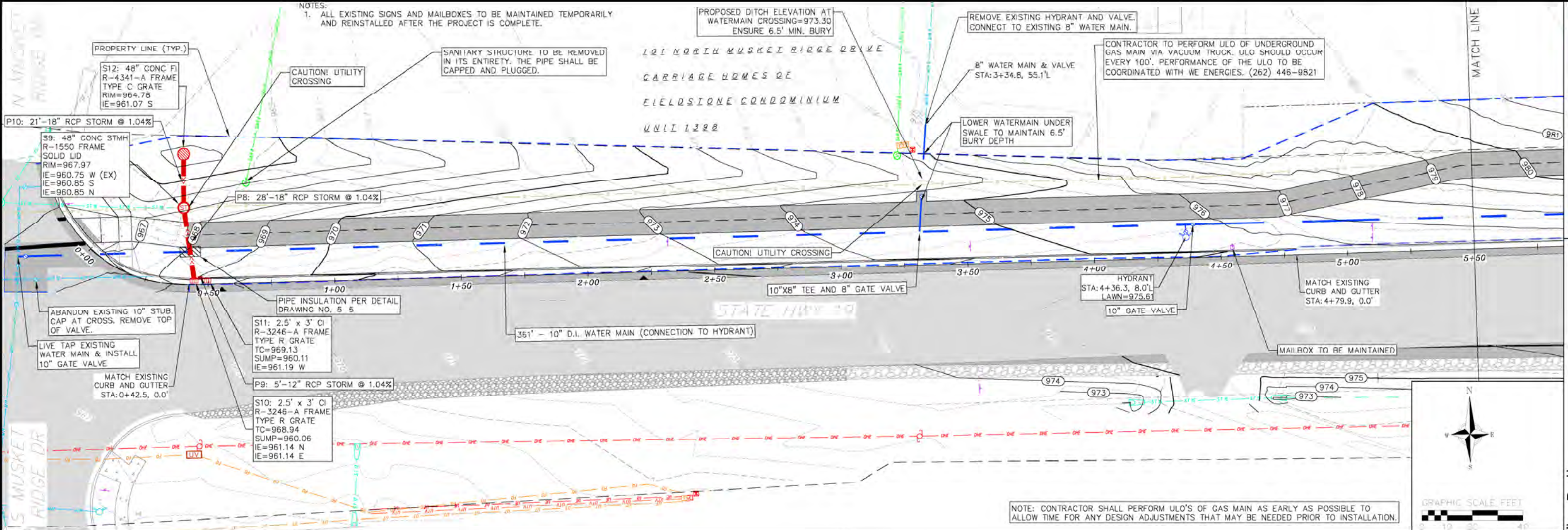
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PROJECT NO.

210178

C4.2

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Plan and Profile - Bike Path
Bike Path & Public Utility Improvements
1050 E Main St
Sun Prairie, Dane County, WI

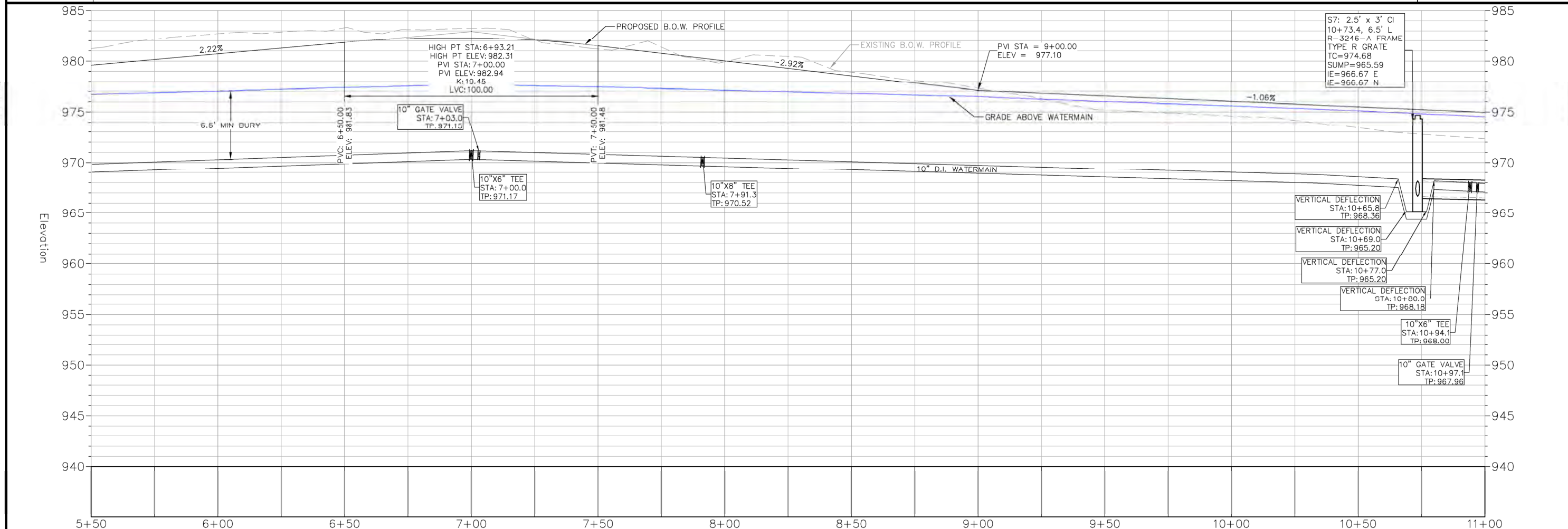
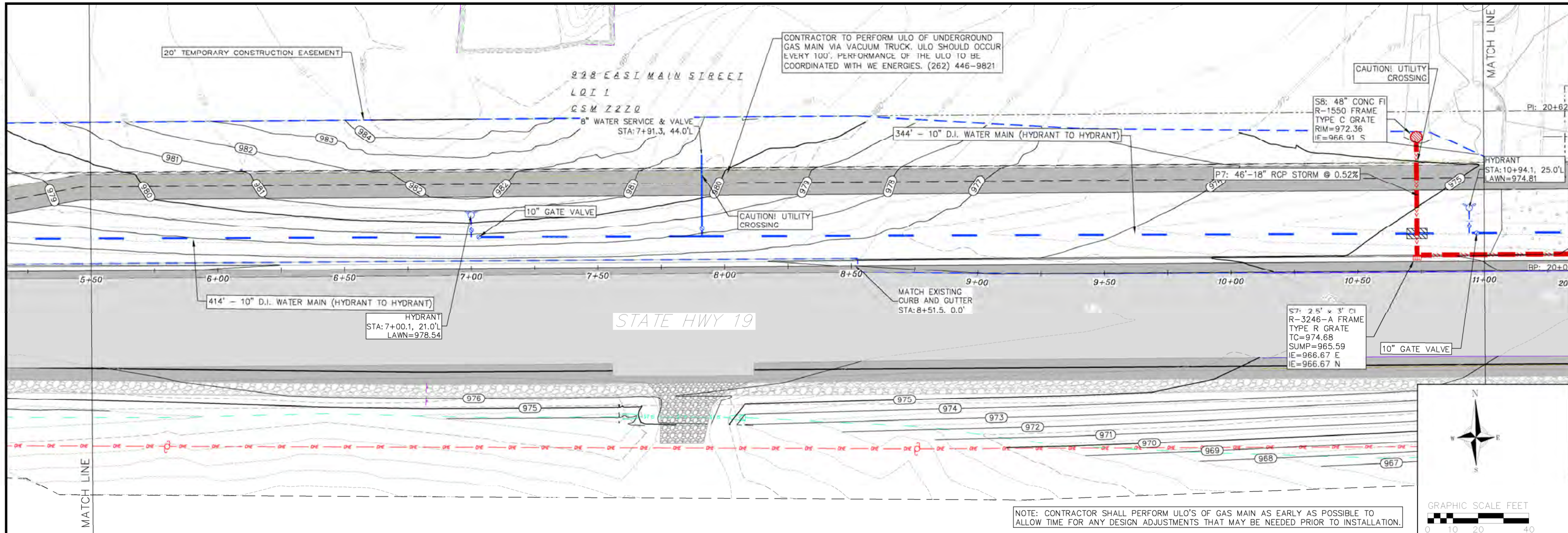
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NO.	DATE	NO.	DATE

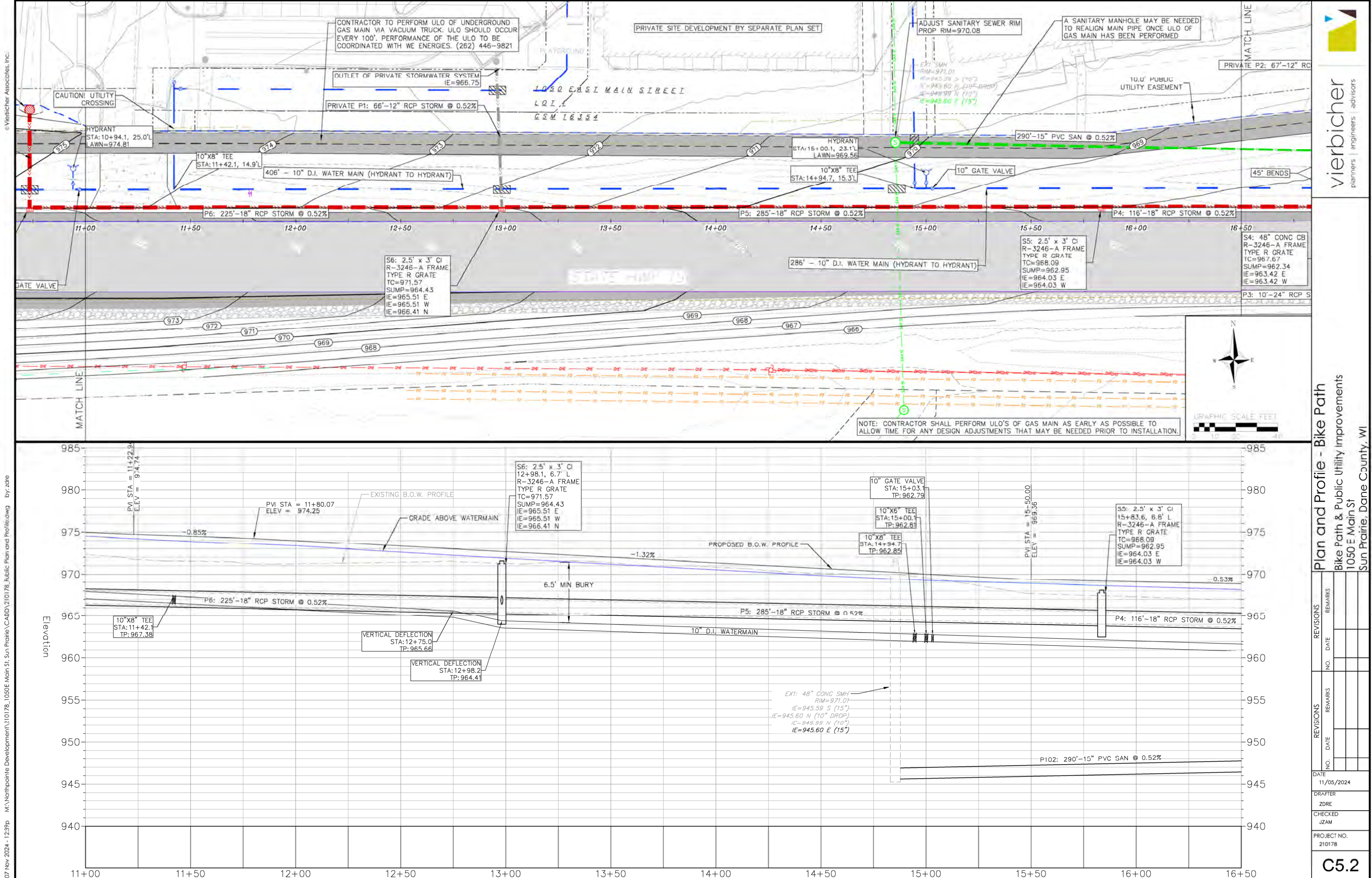
DATE: 11/05/2024
DRAFTER: ZDRE
CHECKED: JZAM
PROJECT NO.: 210178

C5.0


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Plan and Profile - Bike Path

Bike Path & Public Utility Improvements

1050 E Main St

Sun Prairie, Dane County, WI

REVISIONS		REVISIONS	
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DATE

11/05/2024

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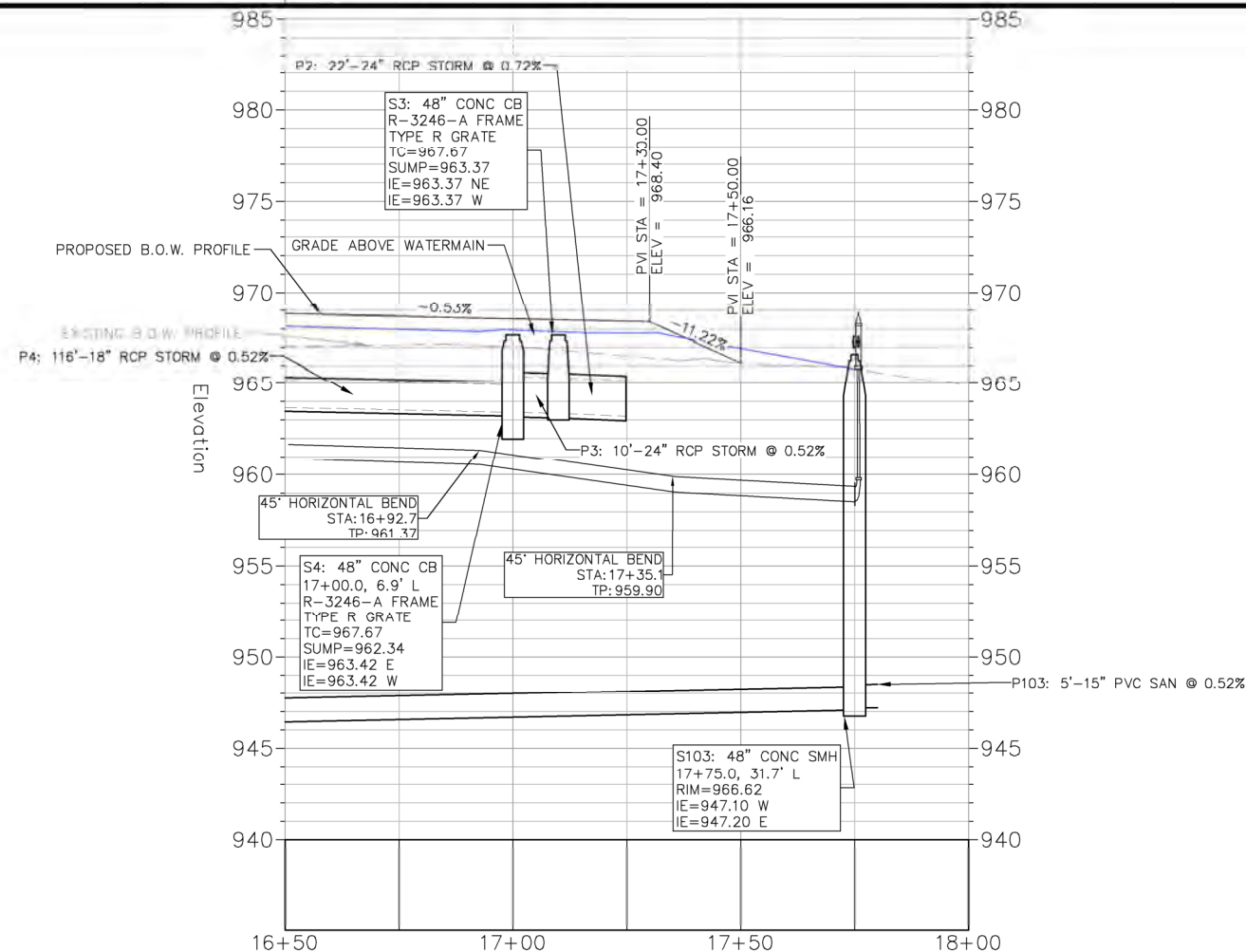
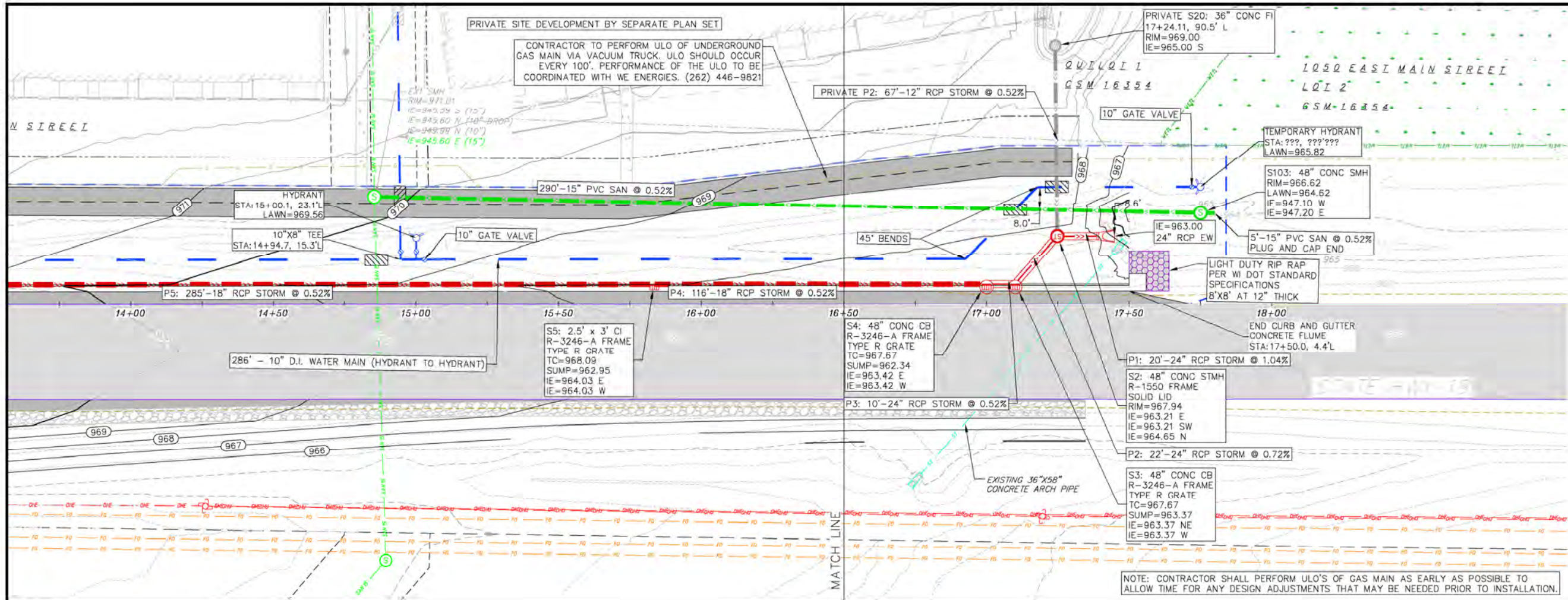
PROJECT NO.


210178

C5.2

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Plan and Profile - Bike Path
Bike Path & Public Utility Improvements
1050 E Main St
Sun Prairie, Dane County, WI

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

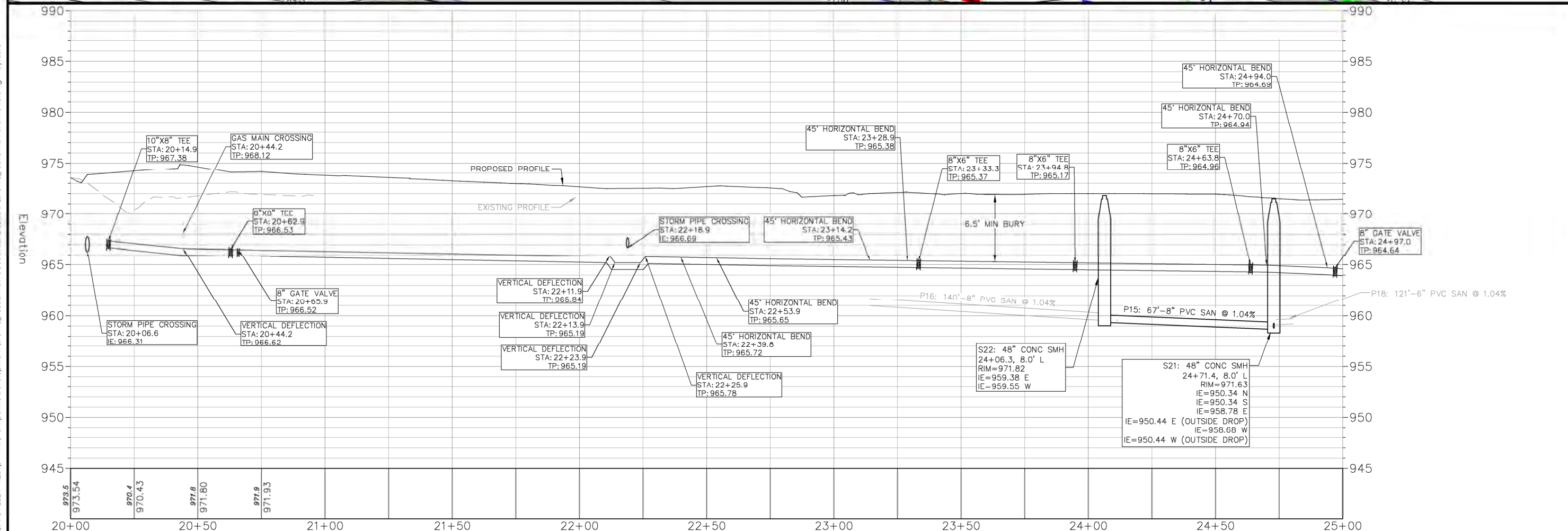
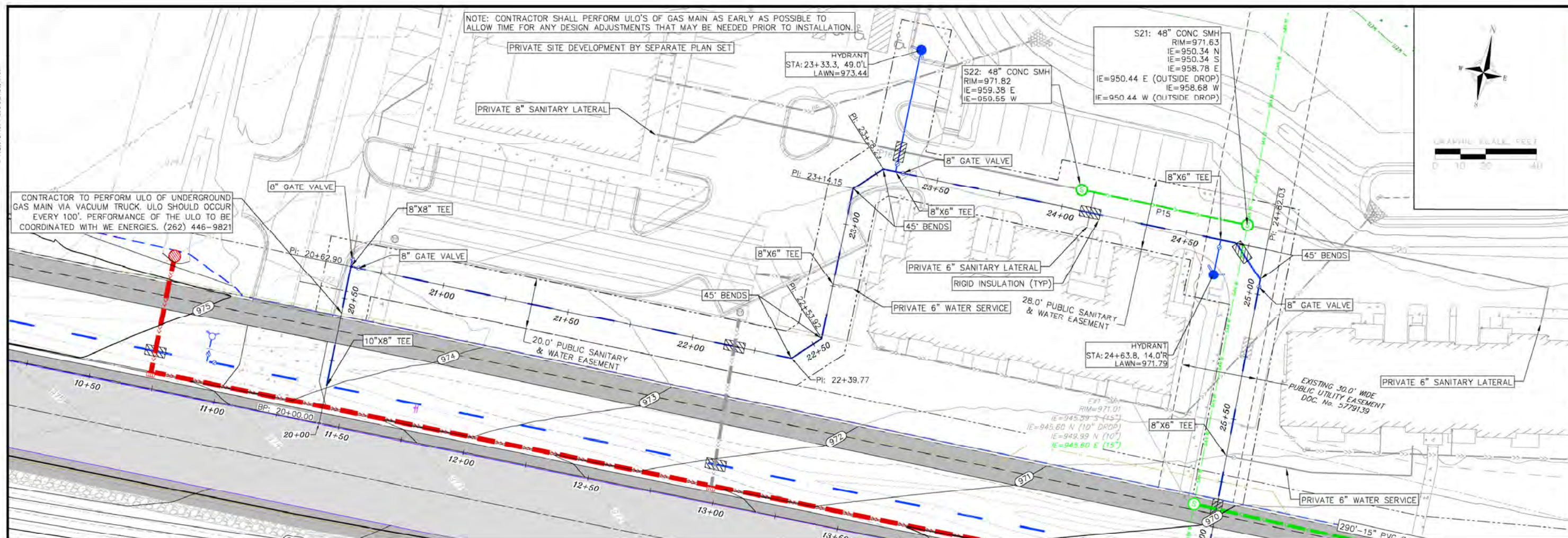
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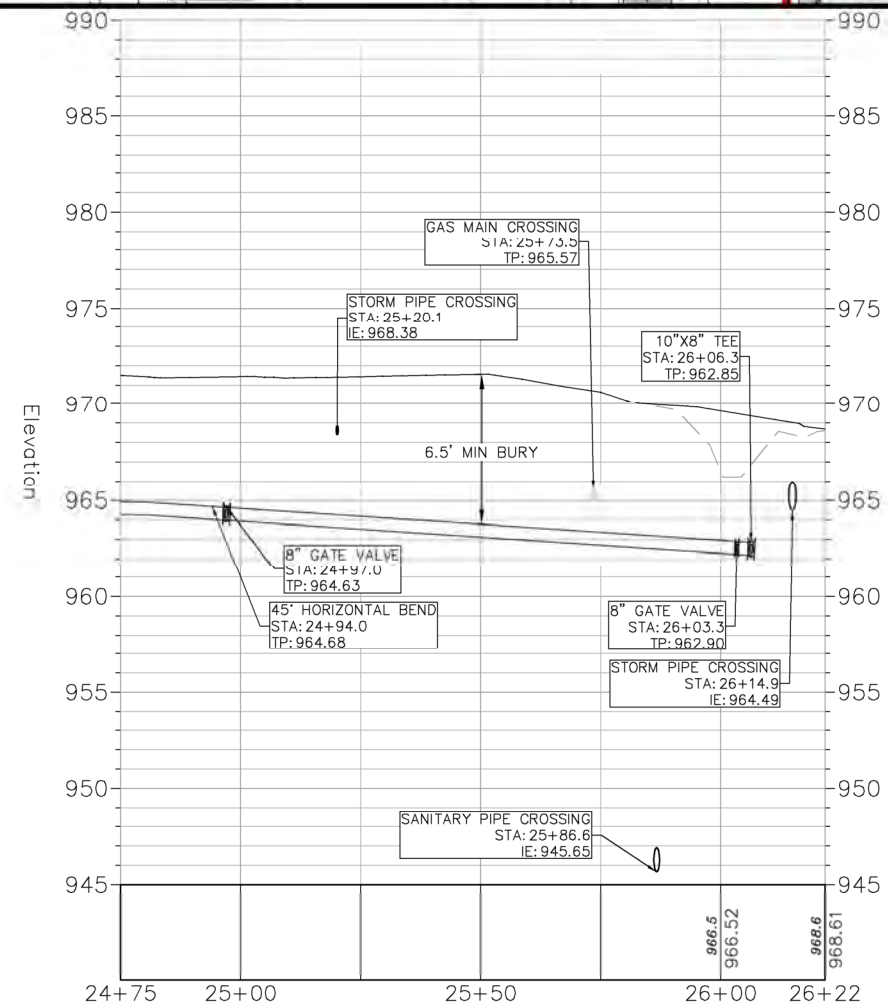
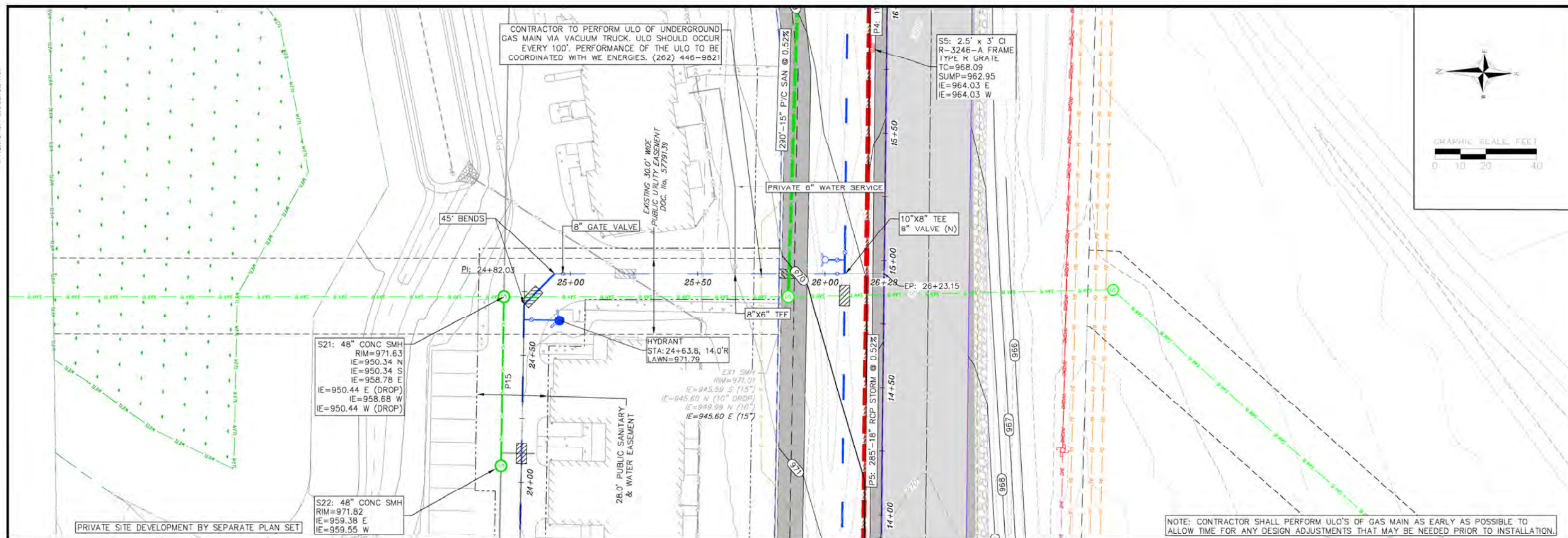
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PROJECT NO.
210178

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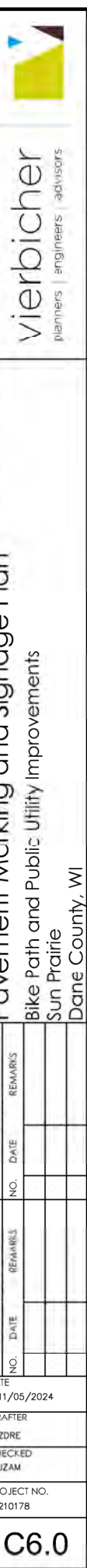


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Plan and Profile - On Site Water & Sanitary

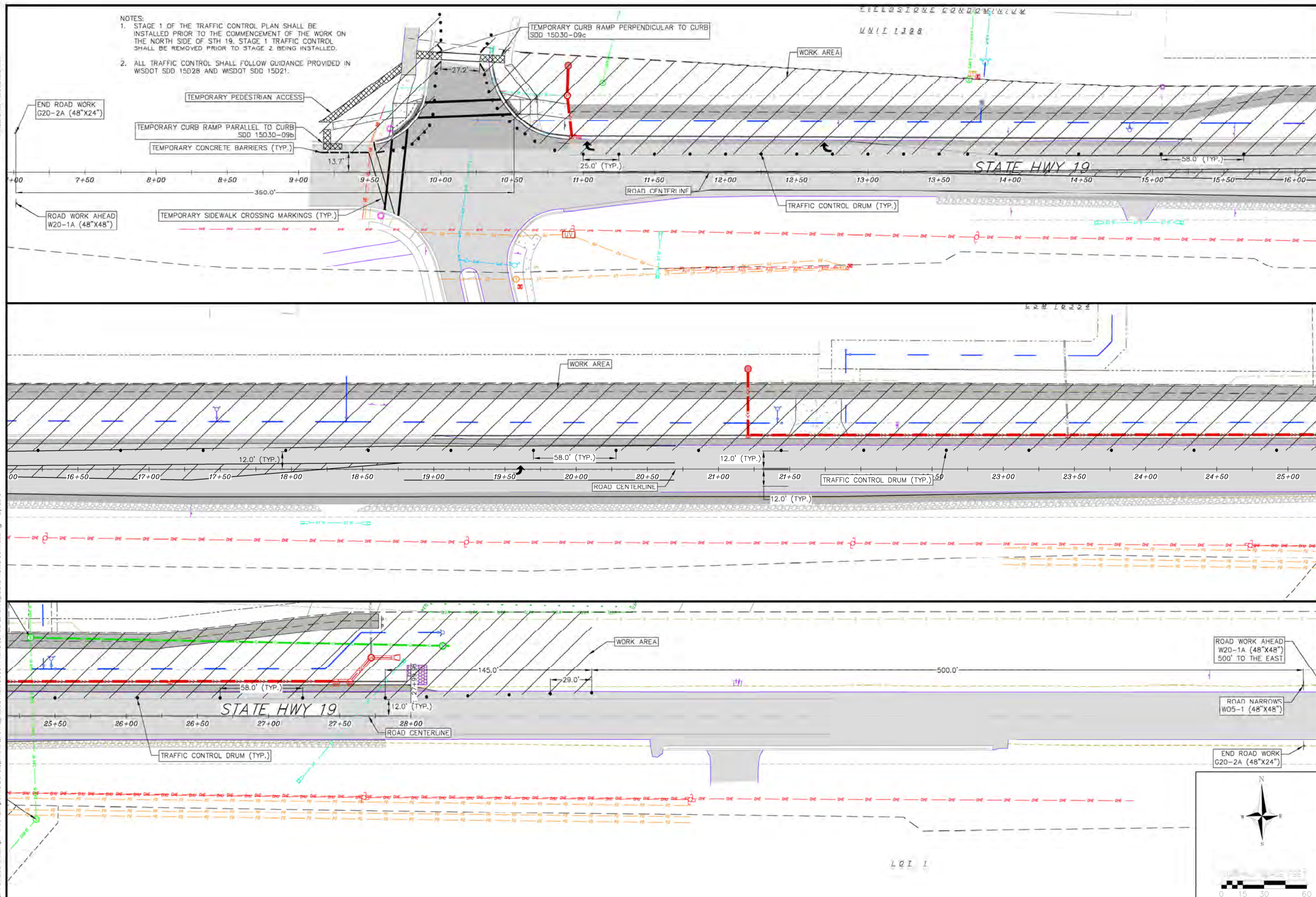
Bike Path & Public Utility Improvements
1050 E Main St
Sun Prairie, Dane County, WI

1030 E Main St
Sun Prairie, Dane County, WI



1. STAGE 1 OF THE TRAFFIC CONTROL PLAN SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF THE WORK ON THE NORTH SIDE OF STH 19. STAGE 1 TRAFFIC CONTROL SHALL BE REMOVED PRIOR TO STAGE 2 BEING INSTALLED.


2. ALL TRAFFIC CONTROL SHALL FOLLOW GUIDANCE PROVIDED IN WISDOT SDD 15D28 AND WISDOT SDD 15D21.



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- NOTES:
1. STAGE 2 OF THE TRAFFIC CONTROL PLAN IS TO BE INSTALLED PRIOR TO COMMENCING WORK ON THE SOUTH SIDE OF STH 19. STAGE 2 TRAFFIC CONTROL SHALL BE REMOVED PRIOR TO STAGE 3 TRAFFIC CONTROL INSTALLATION.
 2. ALL TRAFFIC CONTROL SHALL FOLLOW GUIDANCE PROVIDED IN WISDOT SDD 15028 AND WISDOT SDD 15021.





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Traffic Control Plan - Stage 2
Bike Path & Public Utility Improvements
1050 E Main St
Sun Prairie, Dane County, WI

REVISIONS	
NO.	DATE

REVISIONS	
NO.	DATE

DATE: 11/5/2024

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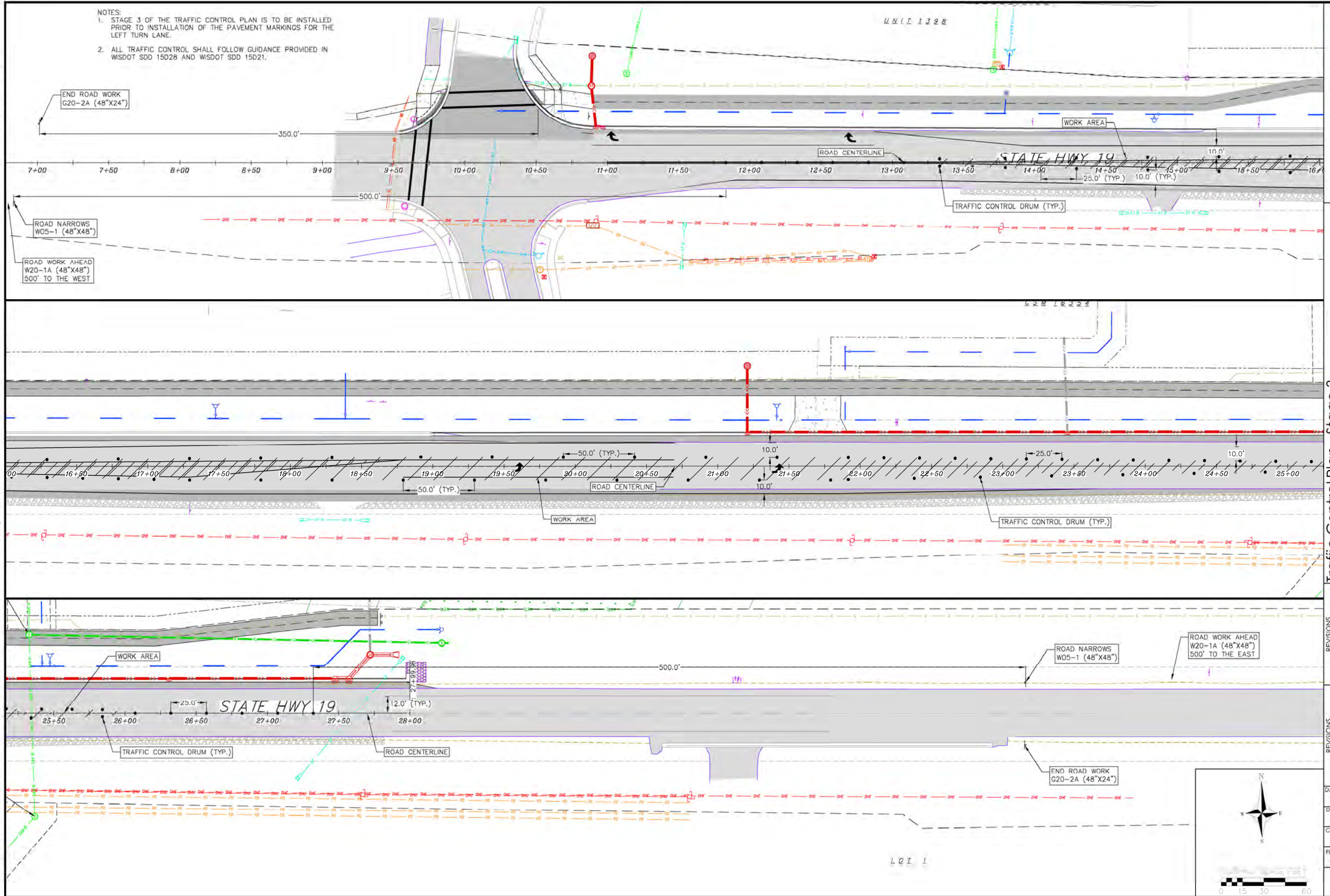
PROJECT NO: 210178

C7.1

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- NOTES:
1. STAGE 3 OF THE TRAFFIC CONTROL PLAN IS TO BE INSTALLED PRIOR TO INSTALLATION OF THE PAVEMENT MARKINGS FOR THE LEFT TURN LANE.
 2. ALL TRAFFIC CONTROL SHALL FOLLOW GUIDANCE PROVIDED IN WISDOT SDD 15D28 AND WISDOT SDD 15D21.



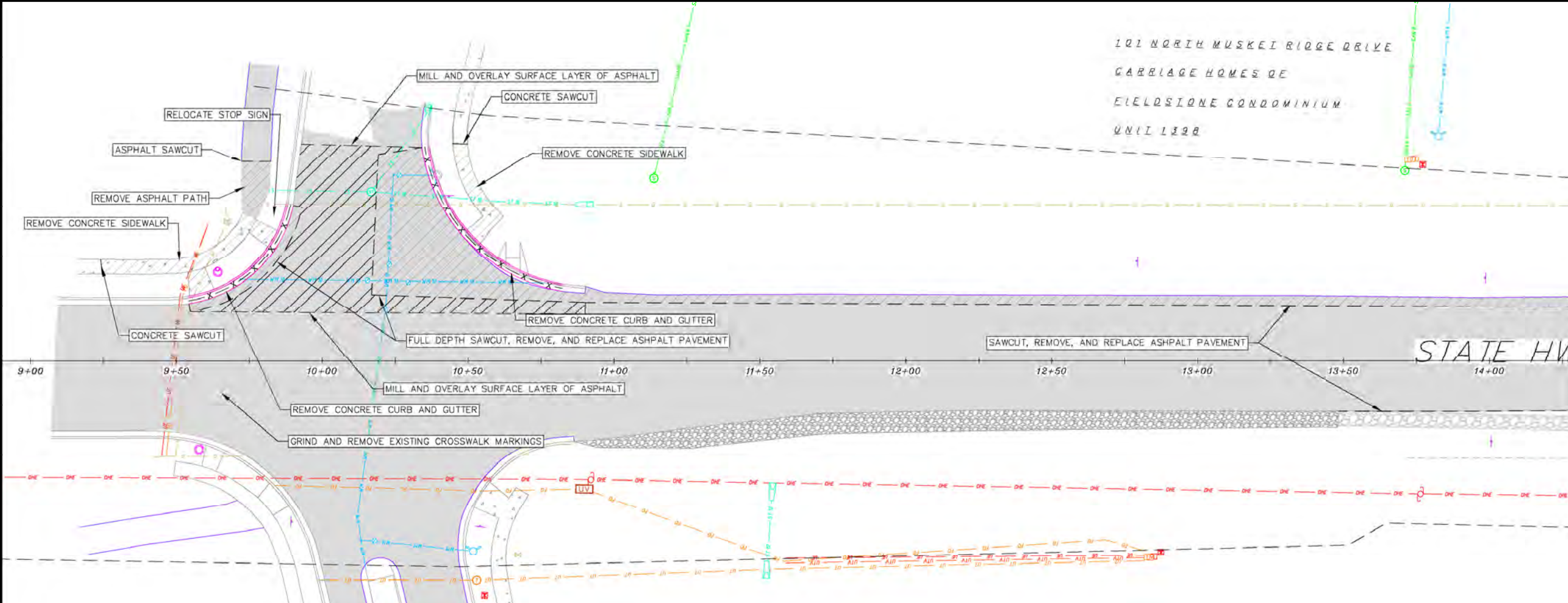
Traffic Control Plan - Stage 3

Bike Path & Public Utility Improvements
1050 E Main St
Sun Prairie, Dane County, WI

REVISIONS		NO.	DATE	REMARKS
REVISIONS		NO.	DATE	REMARKS
DATE		11/5/2024		
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PROJECT NO.		210178		
C7.2				

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Removal Plan
Bike Path and Public Utility Improvements
Sun Prairie
Dane County, WI

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

DATE
11/05/2024

DRAFTER
ZDRE

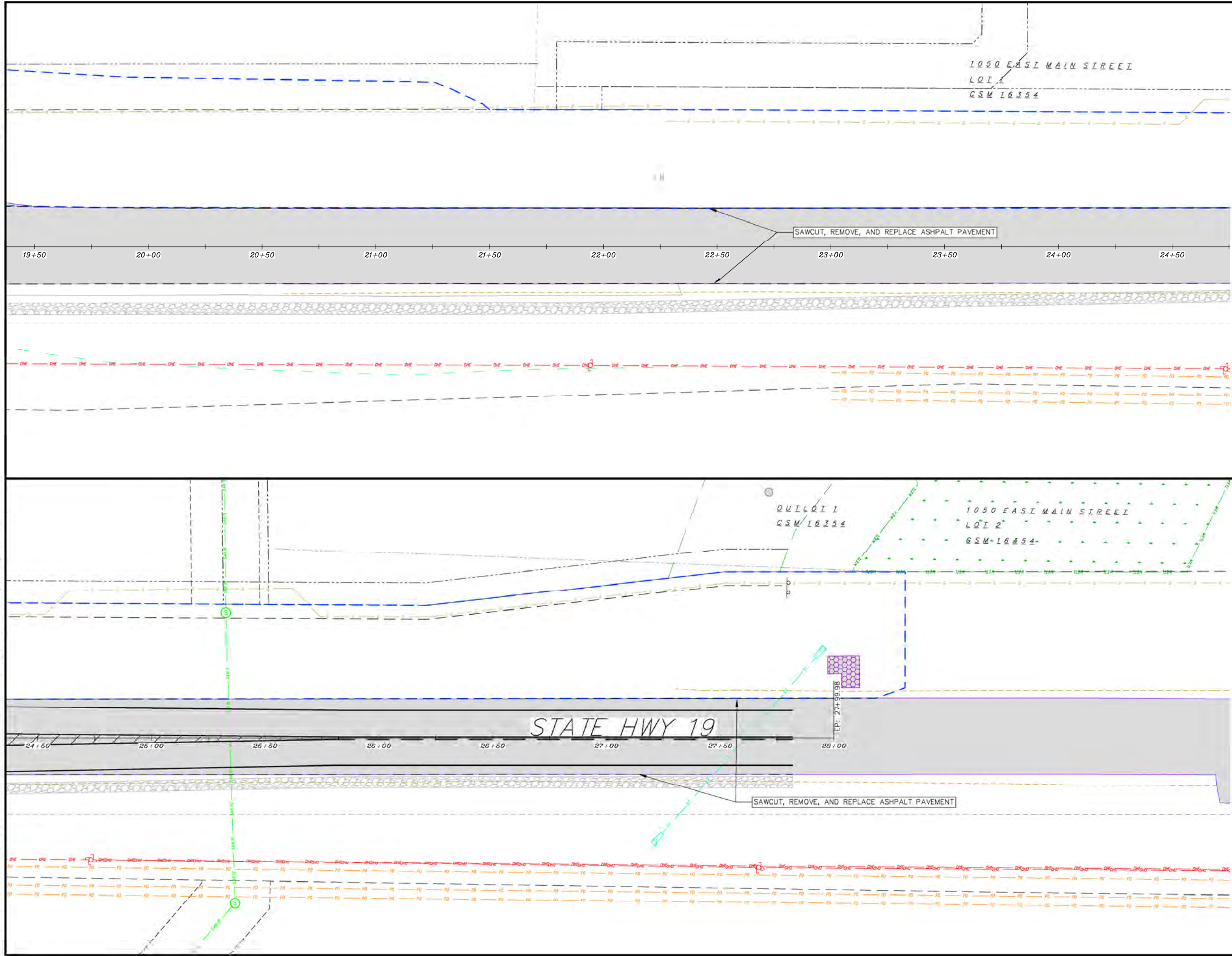
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PROJECT NO.
210178

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Removal Plan

Bike Path and Public Utility Improvements
Sun Prairie
Dane County, WI

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

DATE
11/05/2024

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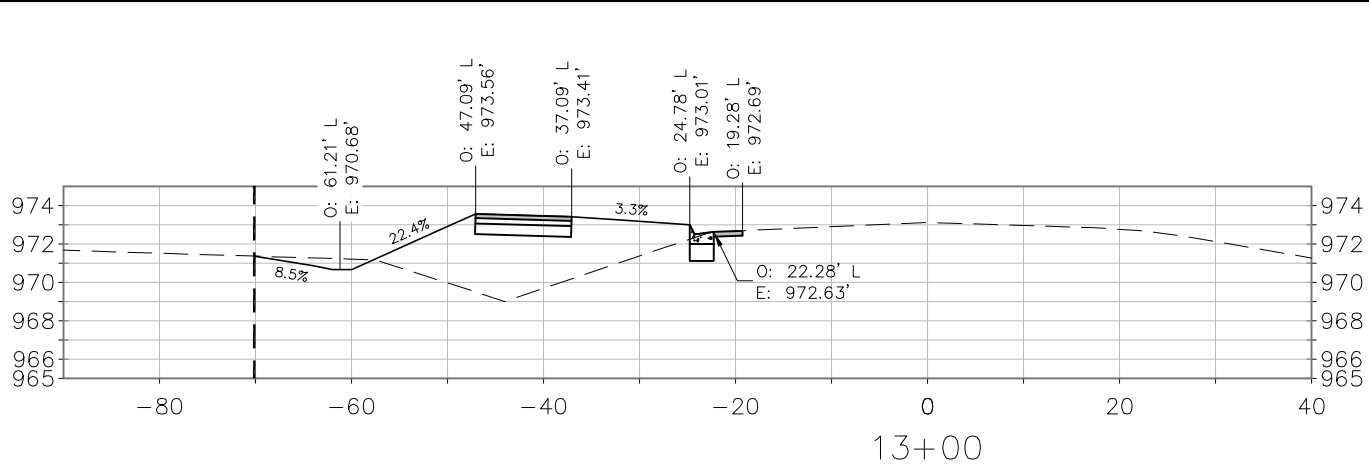
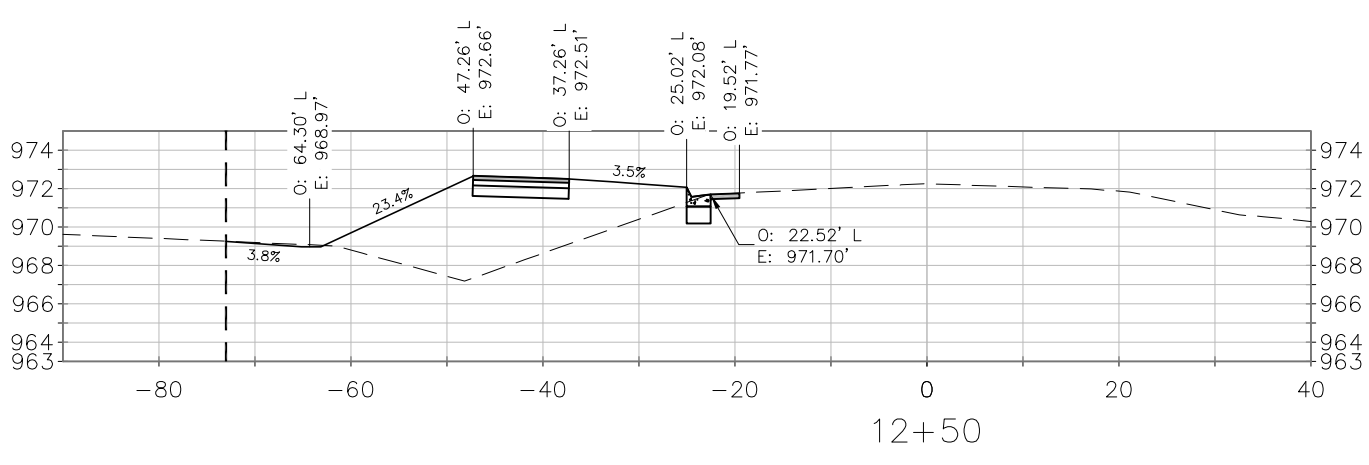
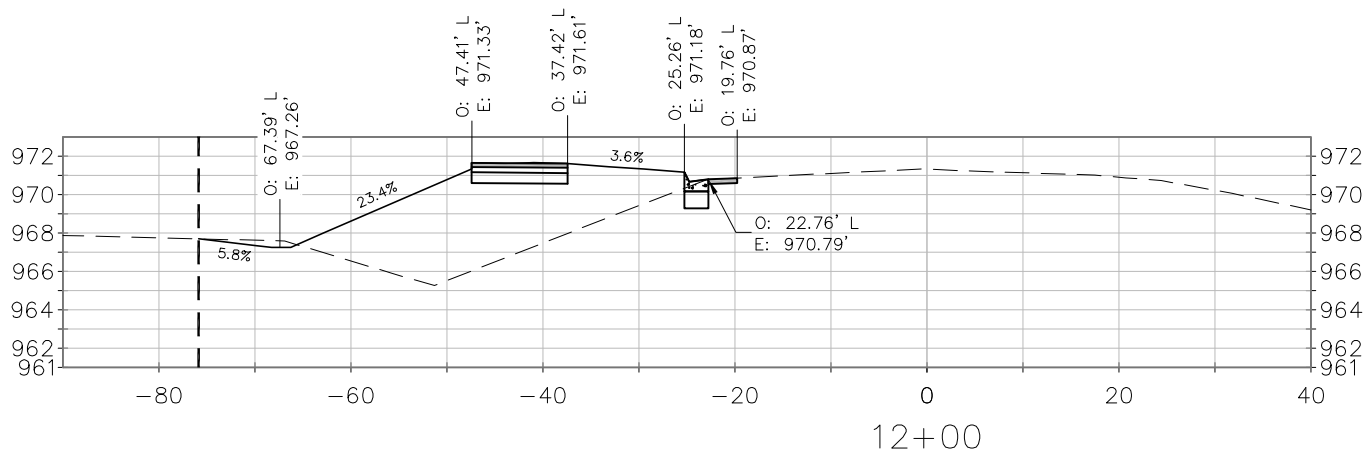
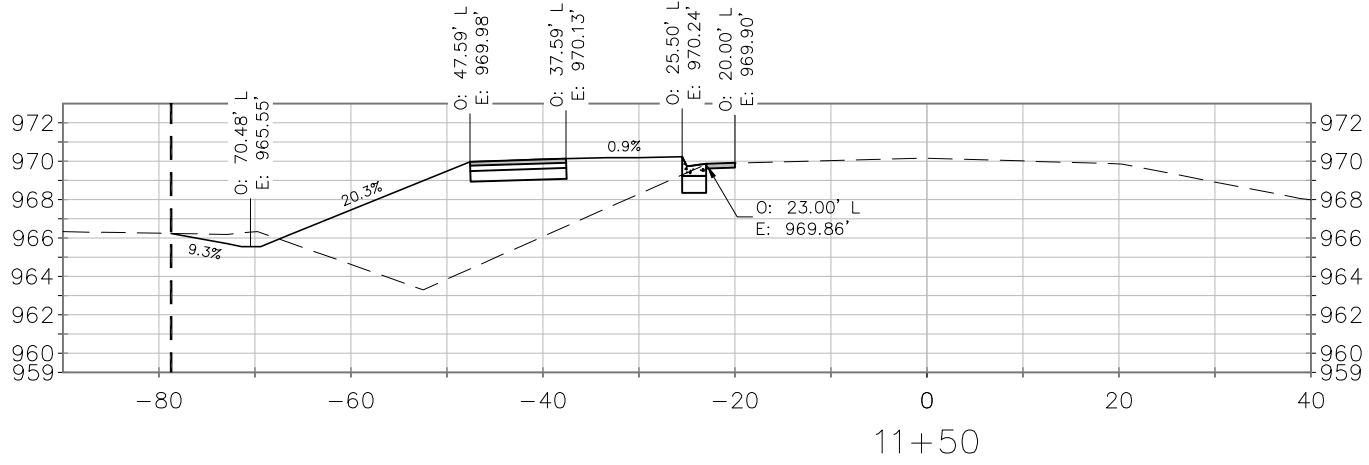
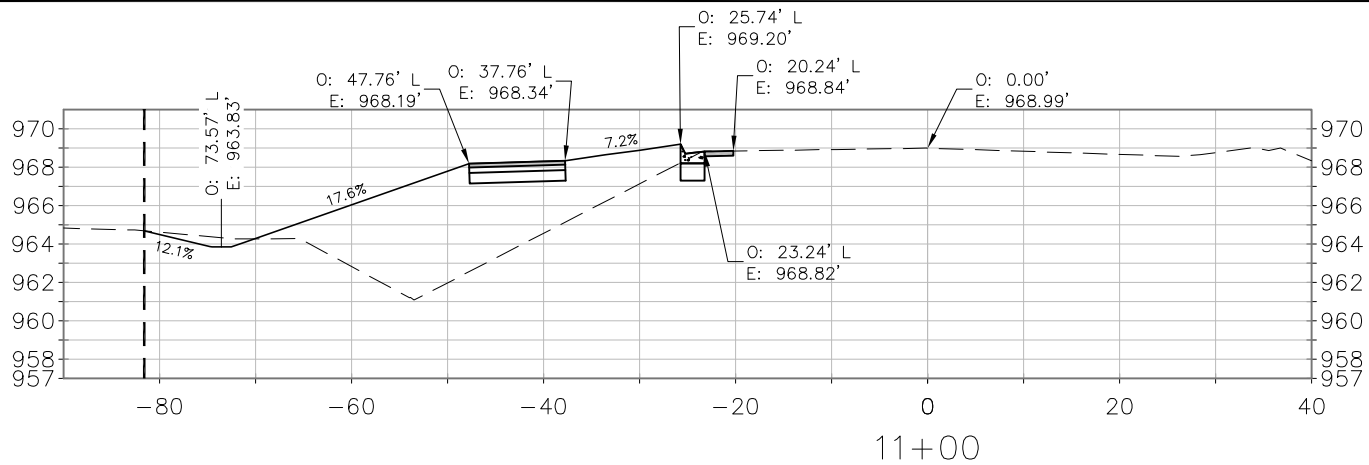
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PROJECT NO.
210178

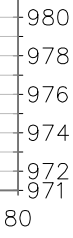
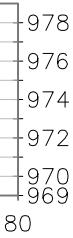
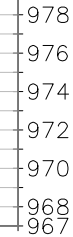
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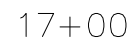
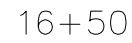
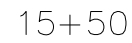
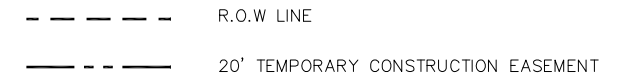
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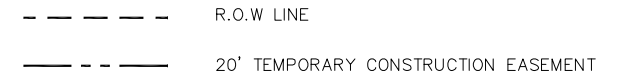
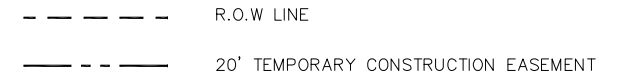
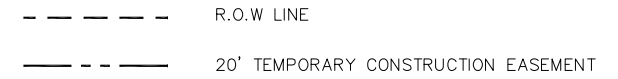
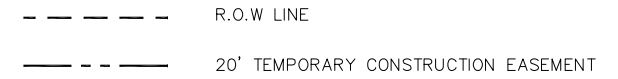


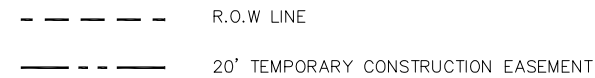
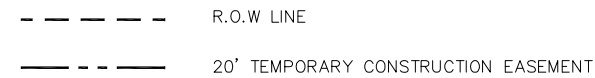
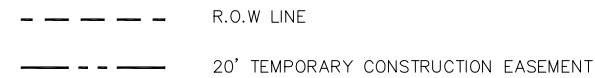
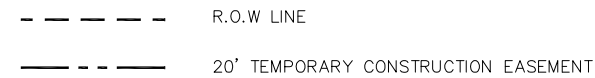
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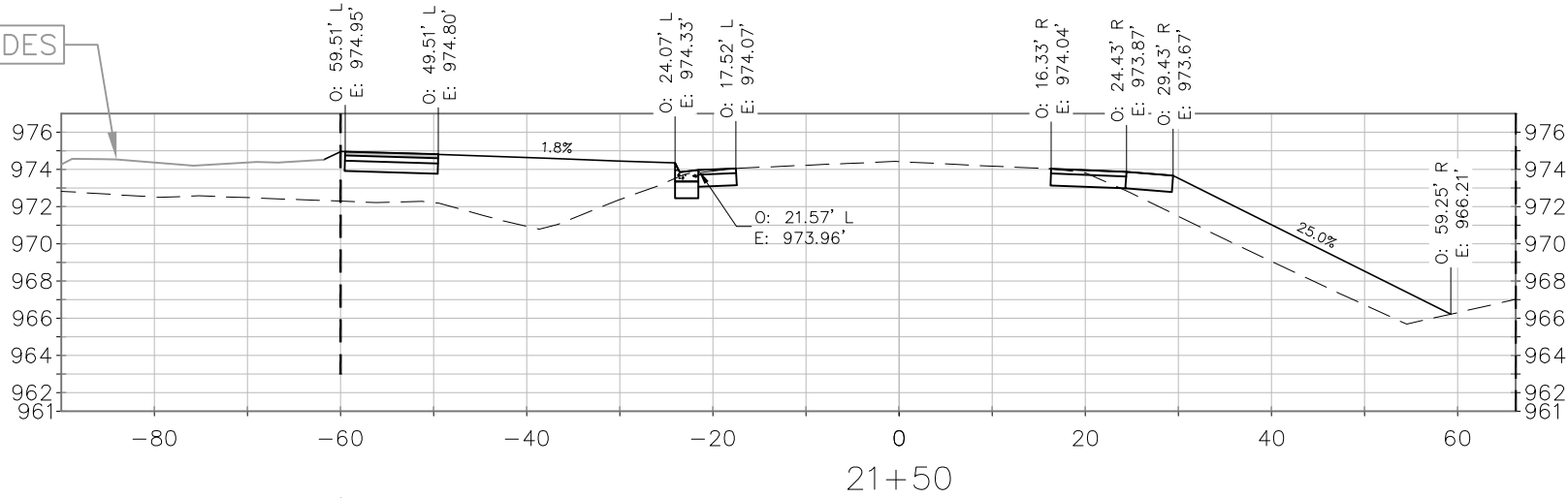
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PROJECT NO.		JZAM	
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C9.2			



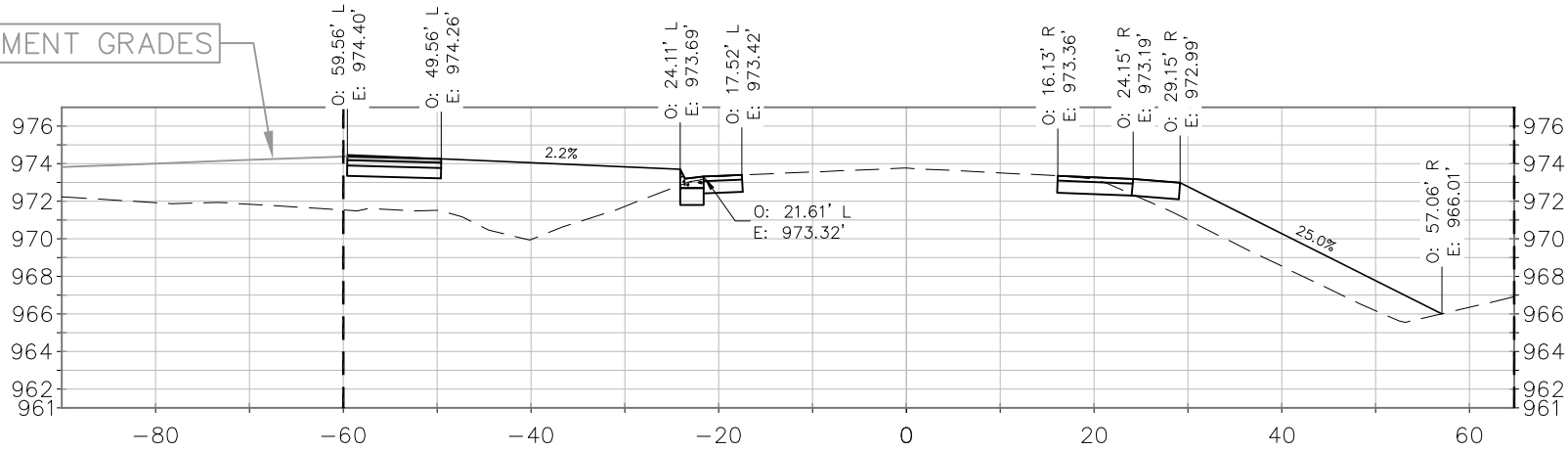


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	REVISIONS		REVISIONS					
	NO.	DATE	REMARKS	NO.	DATE	REMARKS		

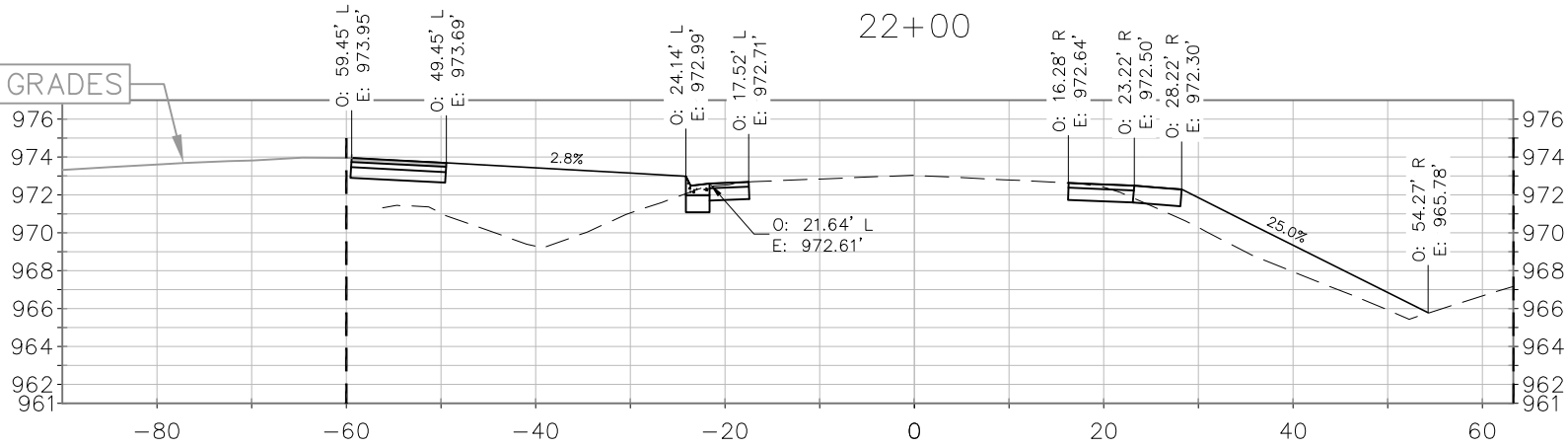
PRIVATE SITE DEVELOPMENT GRADES



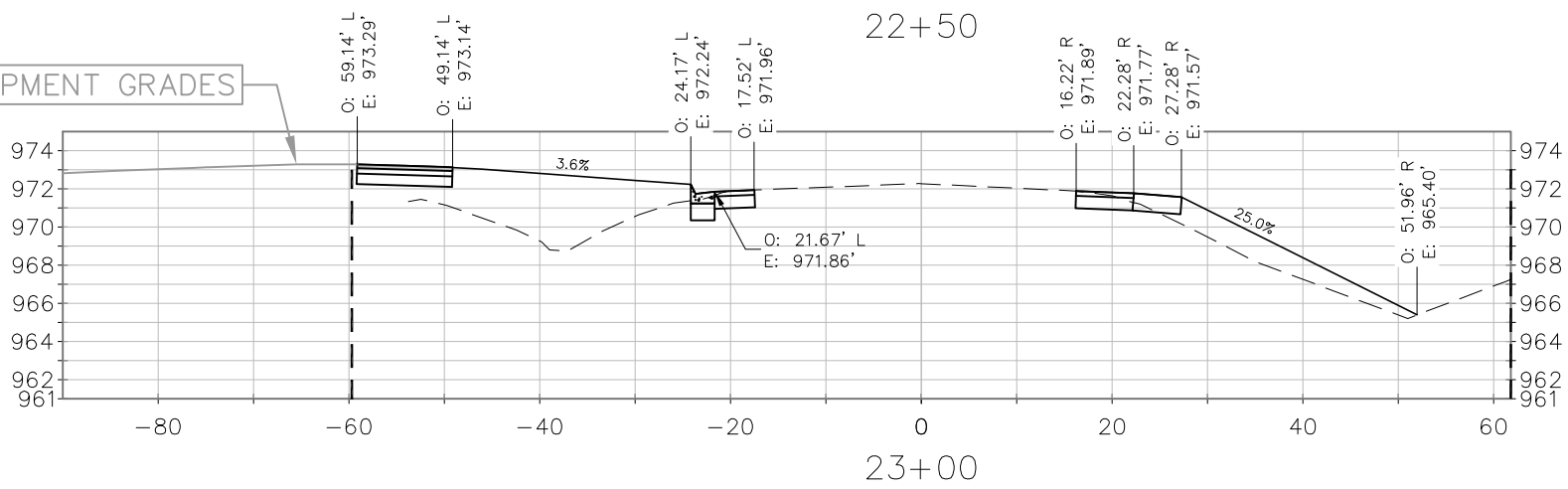
PRIVATE SITE DEVELOPMENT GRADES



PRIVATE SITE DEVELOPMENT GRADES

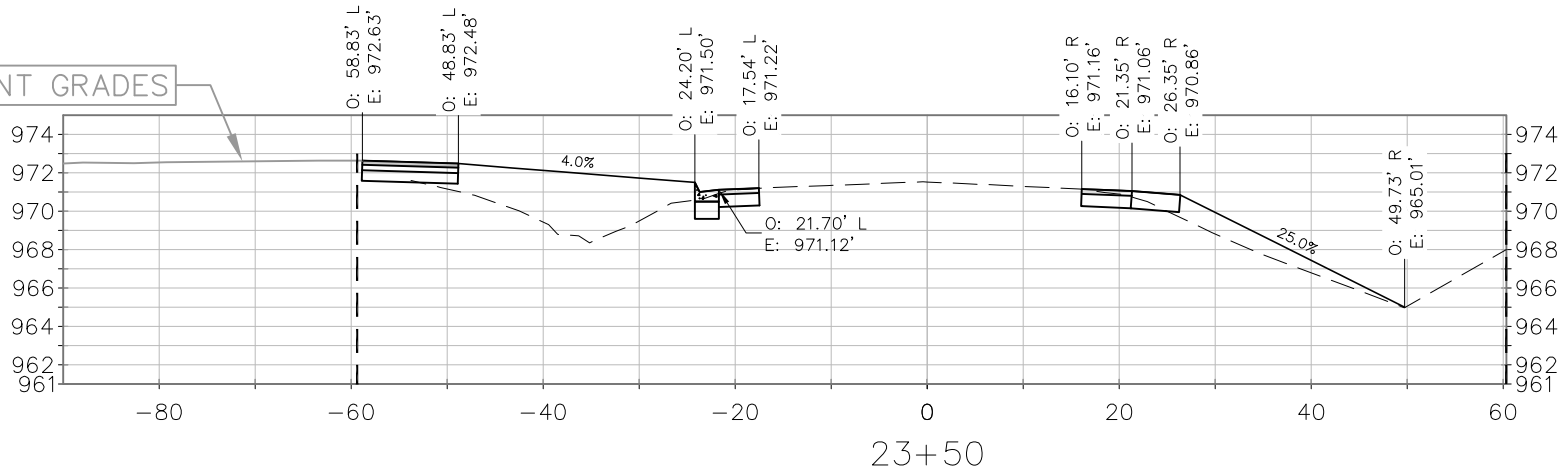


PRIVATE SITE DEVELOPMENT GRADES

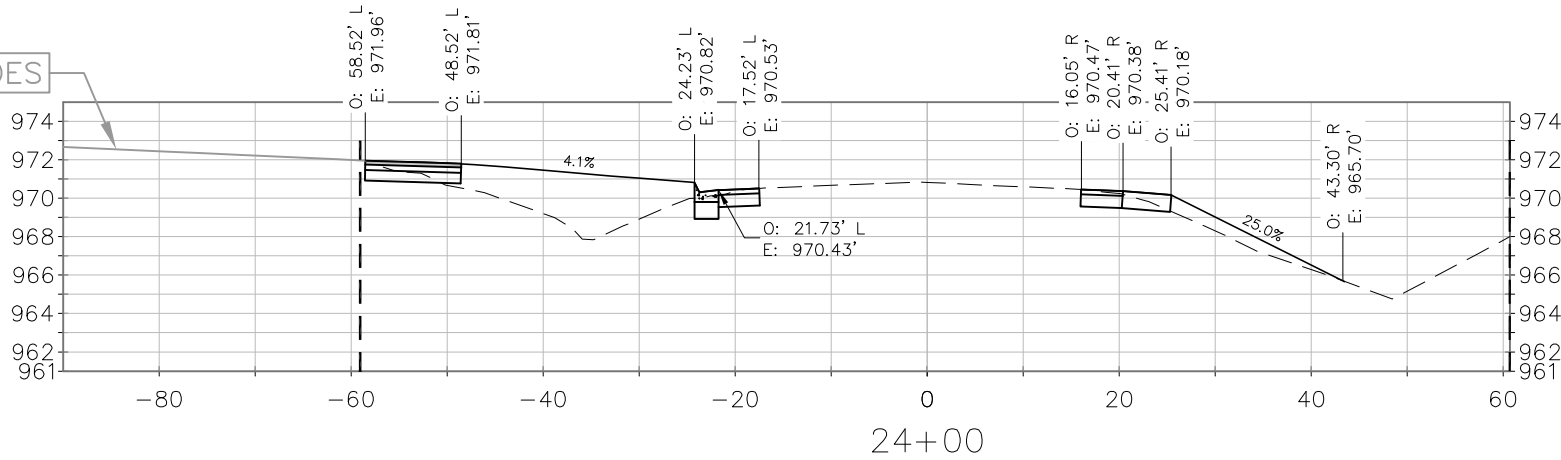


--- R.O.W LINE

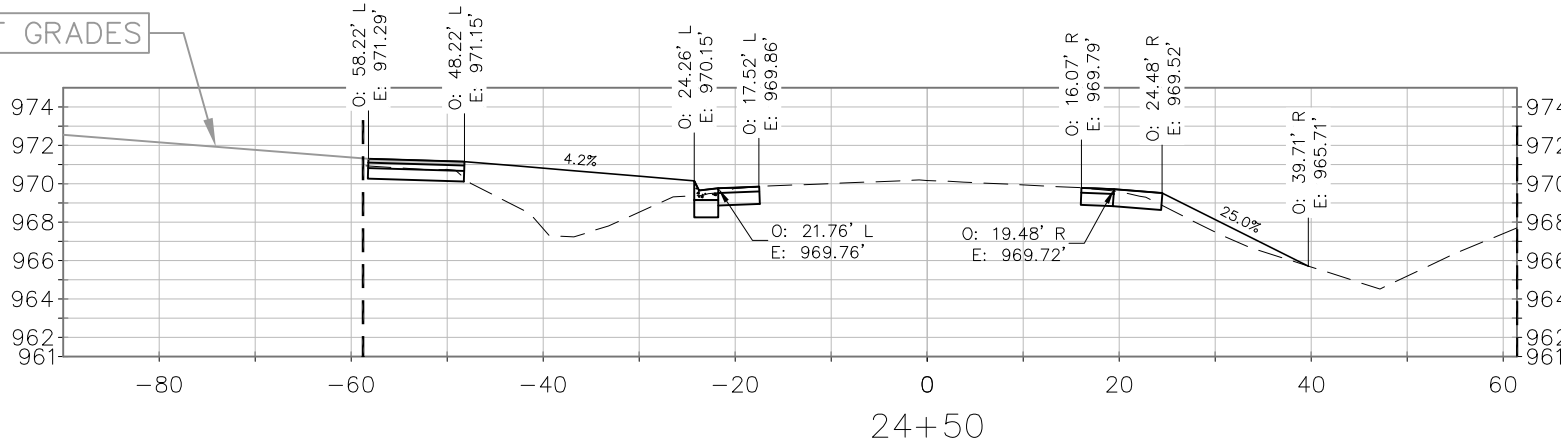
PRIVATE SITE DEVELOPMENT GRADES



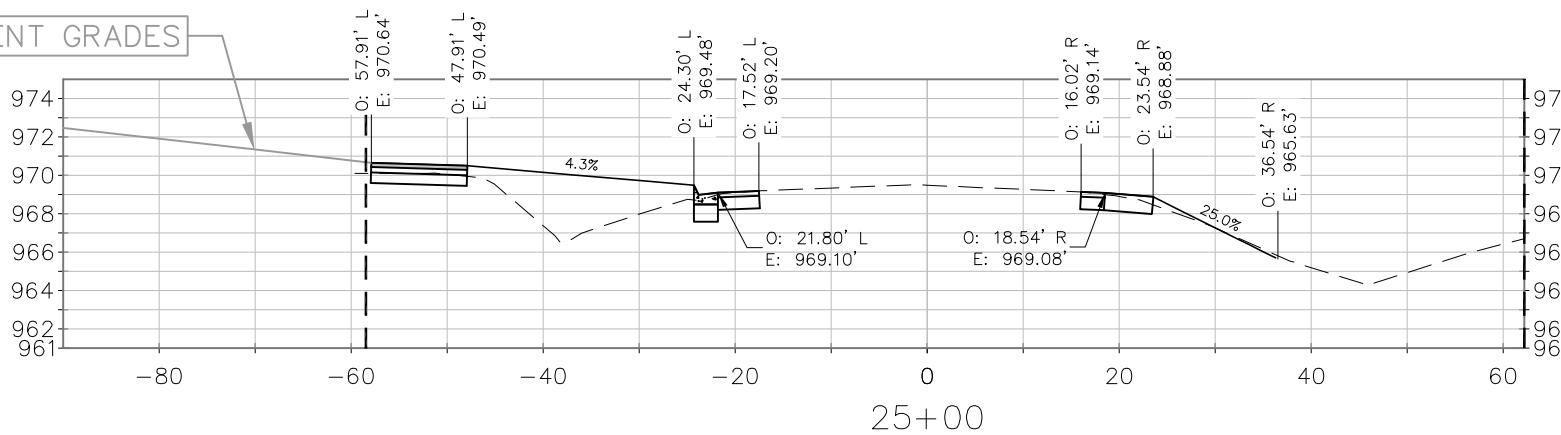
PRIVATE SITE DEVELOPMENT GRADES



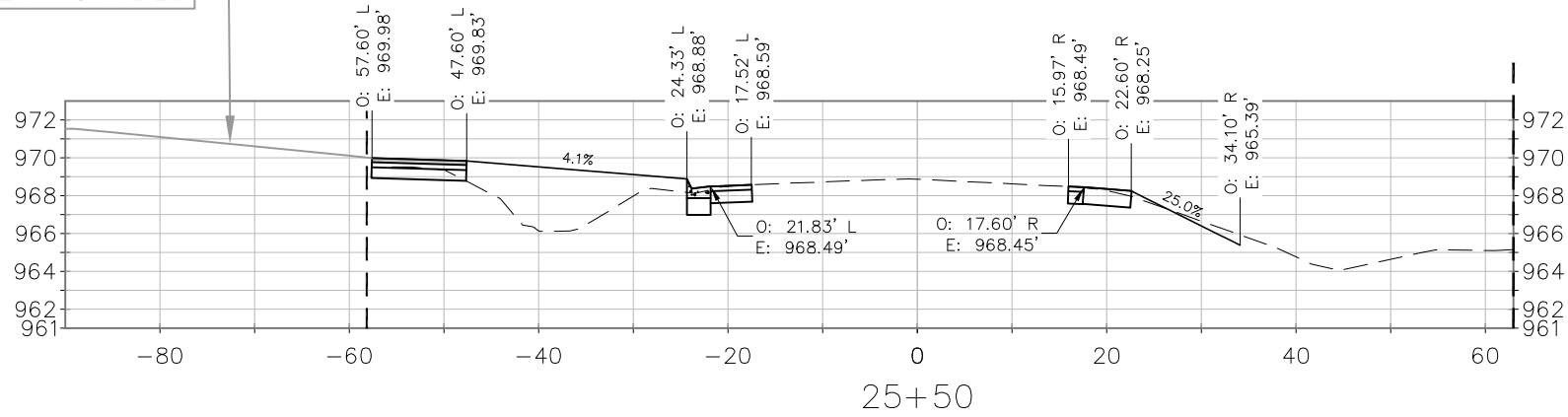
PRIVATE SITE DEVELOPMENT GRADES



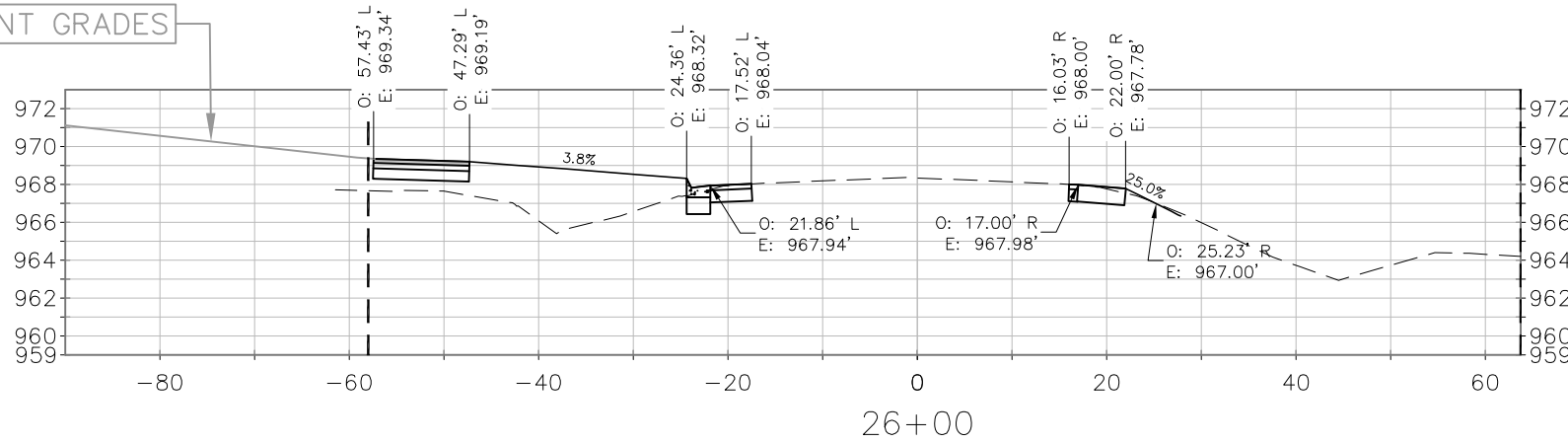
PRIVATE SITE DEVELOPMENT GRADES



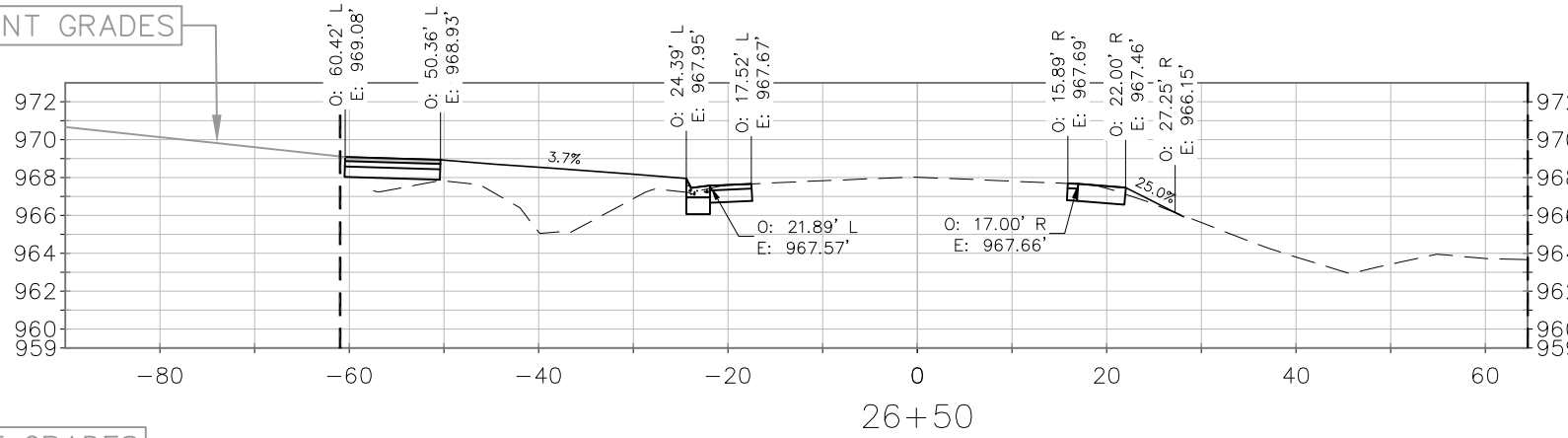
PRIVATE SITE DEVELOPMENT GRADES



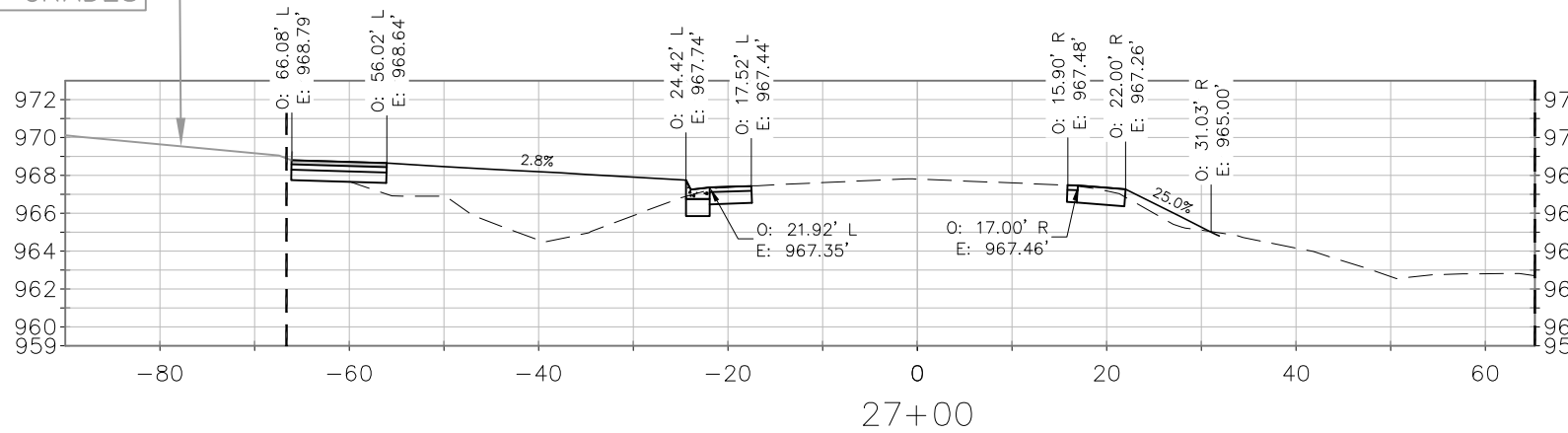
PRIVATE SITE DEVELOPMENT GRADES



PRIVATE SITE DEVELOPMENT GRADES



PRIVATE SITE DEVELOPMENT GRADES



--- R.O.W. LINE



vierbicher
engineers advisors

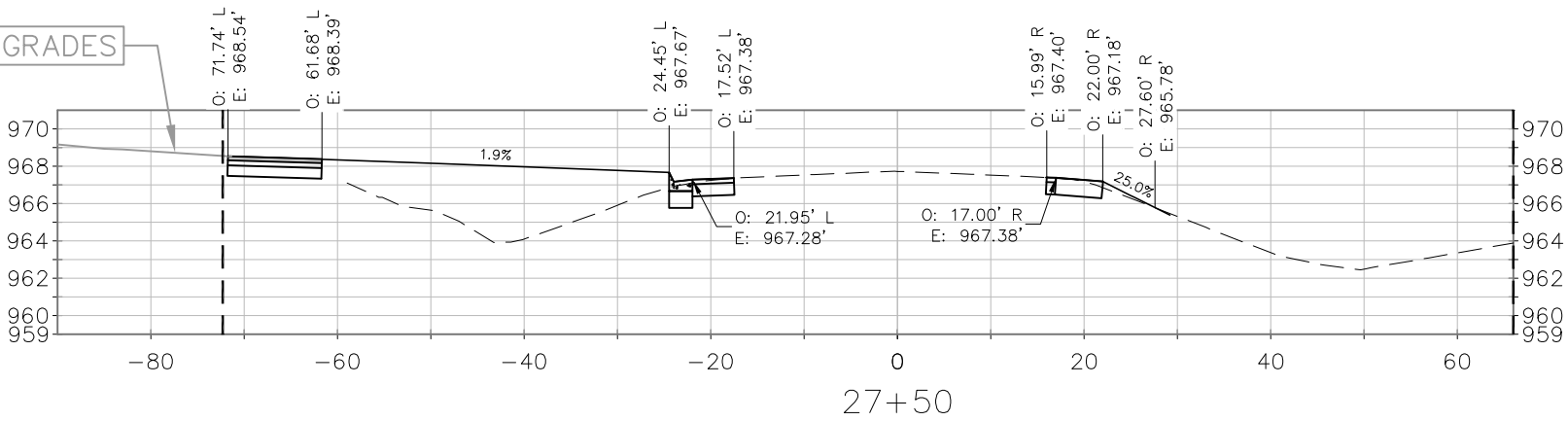
Cross Sections
Bike Path and Public Utility Improvements
Sun Prairie
Dane County, WI

REVISIONS		REVISIONS	
NO.	DATE	NO.	DATE

DATE	11/05/2024
DRAFTER	ZDRE
CHECKED	JZAM
PROJECT NO.	210178

C9.7

PRIVATE SITE DEVELOPMENT GRADES



--- R.O.W LINE

C9.8	PROJECT NO. 210178		CHECKED JZAM		DRAFTER ZDRE		DATE 11/05/2024	

Cross Sections
Bike Path and Public Utility Improvements
Sun Prairie
Dane County, WI