LAX

JANUARY 2018

PROJECT ID: 5991-02-54, 5991-02-55

WITH: 5991-02-57

COUNTY: LA CROSSE

ORDER OF SHEETS

TOTAL SHEETS = 100

09

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5991-02-54	WISC 2018042	1
5991-02-55	WISC 2018043	1

CITY OF ONALASKA, BRAUND STREET

STH 16 TO PH

LOCAL STREET

LA CROSSE COUNTY

STATE PROJECT NUMBER 5991-02-54

CITY OF ONALASKA, PH

BRAUND ST TO THEATER RD

LOCAL STREET

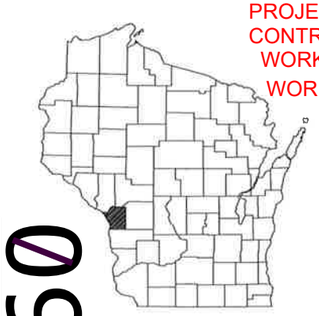
LA CROSSE COUNTY

STATE PROJECT NUMBER 5991-02-55

AS-BUILT PLAN

SUPERVISOR: Jim Savoldelli
PROJECT MANAGER: Brian Meyer
PROJECT LEADER: Lorie Peterson
CONTRACTOR: Gerke Excavating, Inc.
WORK STARTED: 4/23/2018
WORK COMPLETED 8/31/2018

Subcontractor List
Barricade Flasher Service
Brickline, Inc.
Chippewa Concrete Services, Inc.
Hard Rock Sawing & Drilling
Heider & Bott Co.
Mathy Construction Company
Poellinger Electric, Inc.
Smith Restorations, Inc.



N

Sheets Revised: 5,6,7,8, 9, 14, 15,23, 28, 30, 31

DESIGN DESIGNATION

		BRAUND	PH
A.A.D.T. (2018)	=	4000	5000
A.A.D.T. (2038)	=	4700	5800
D.H.V.	=	665	865
D.D.	=	59/41	59/41
T.	=	2.5%	2.8%
DESIGN SPEED	=	30 MPH	30 MPH
ESALS	=	408,800	408,800

CONVENTIONAL SYMBOLS

PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- TEMPORARY LIMITED EASEMENT
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

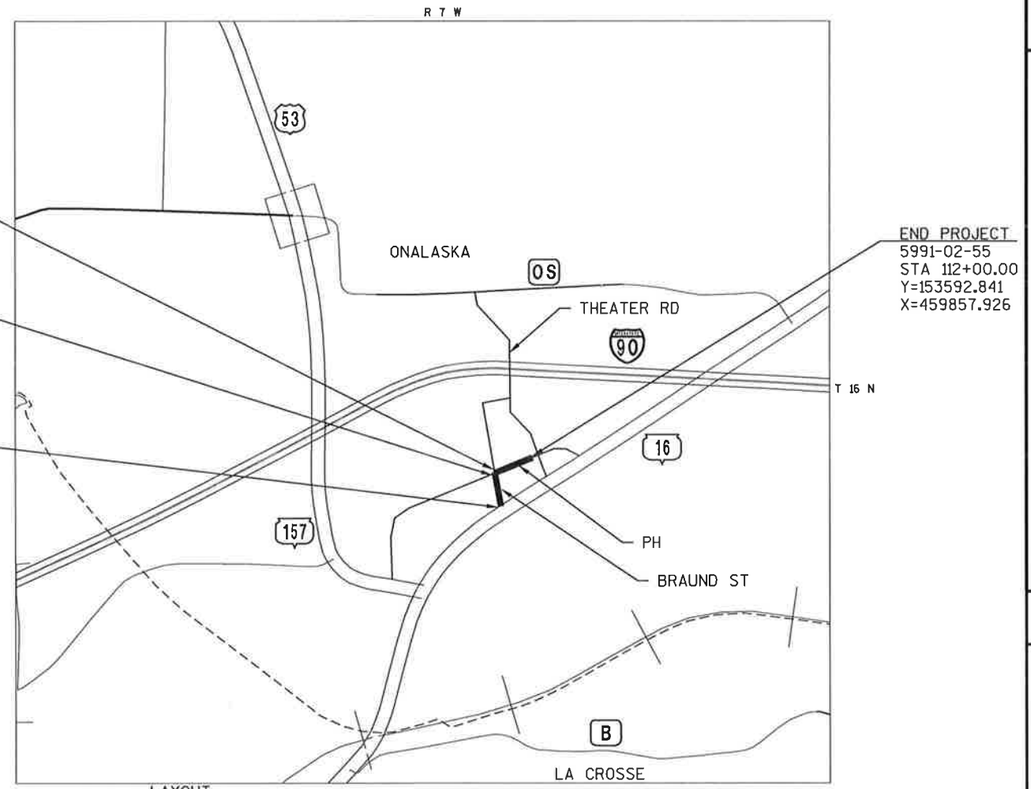
UTILITIES
- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

BEGIN PROJECT
5991-02-55
STA 100+61.34
Y=153295.523
X=458758.766

END PROJECT
5991-02-54
STA 20+86.19
Y=153356.743
X=458825.888

BEGIN PROJECT
5991-02-54
STA 11+46.22
Y=152421.594
X=458847.940

END PROJECT
5991-02-55
STA 112+00.00
Y=153592.841
X=459857.926



LAYOUT

SCALE 0 0.5 MILES

TOTAL NET LENGTH OF CENTERLINE = 0.394 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, LA CROSSE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ACCEPTED FOR
CITY OF ONALASKA

7-17-17 (DATE) C. Jahnke (CITY ENGINEER)

ORIGINAL PLANS PREPARED BY

SEH

WISCONSIN
JEREMY J TOMESH
E-398986
LA CROSSE
WI
PROFESSIONAL ENGINEER

7/14/2017 (Date) Jeremy J. Tomesh (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor SEH
Designer SEH
Management Consultant KL Engineering

APPROVED FOR THE DEPARTMENT
DATE: 7/31/17
Management Consultant Signature

E

FILE NAME : \\SEHLX1\PROJECTS\K0\O\ONALA\133149\C3D\SHEETSPLAN\BRAUND\010101 TI.DWG
LAYOUT NAME - 010101 TI - TITLE SHEET 1 IN EQ 2 MI

PLOT DATE : 1/3/2017 3:36 PM PLOT BY : TOREY LEONARD PLOT NAME :

WISDOT/CADDS SHEET 10

GENERAL NOTES

1. NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.
2. THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
3. CROSS SECTIONS SHOWN INCLUDE THE THICKNESS OF TOPSOIL WHERE REQUIRED. TOPSOIL SHALL BE REPLACED TO A 4-INCH TYPICAL DEPTH.
4. TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
5. THE EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
6. ASPHALTIC AND CONCRETE SURFACES SHALL BE SAWCUT AT THE MATCH LINE AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
7. DISTURBED AREAS WITHIN THE RIGHT OF WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE TOPSOILED, FERTILIZED, SEEDED AND MULCHED.
8. STATIONING, DISTANCES AND OFFSETS FOR SIGNS SHOWN ON THE PLANS ARE APPROXIMATE AND THE FINAL LOCATIONS OF SIGNS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.
9. A CONVERSION FACTOR OF 2.0 TONS/CY IS USED TO ESTIMATE QUANTITIES FOR BASE AGGREGATE DENSE 1 1/4-INCH.
10. A CONVERSION FACTOR OF 112 LBS/IN/SY IS USED TO ESTIMATE QUANTITIES FOR HMA PAVEMENT.
11. MAINTAIN EXISTING GUTTER FLOW AS INDICATED BY THE GRADES AND FLOW ARROWS SHOWN ON THE DRAWINGS. ADJUST BACK OF CURB AND FLAG OF CURB GRADES SO THAT THE CURB & GUTTER GRADES DO NOT EXCEED THE GRADES SHOWN ON THE TYPICAL RAMP SECTION.
12. CURB TAPERS AT RAMPS ARE 2 FEET LONG UNLESS OTHERWISE NOTED.
13. CONSTRUCTION OF TEMPORARY ACCESS POINTS TO RESIDENCES AND BUSINESS IS INCIDENTAL TO THE PROJECT.
14. INLET PROTECTION TYPE C IS UTILIZED THROUGHOUT THE PROJECT. SEE MISCELLANEOUS QUANTITY TABLE FOR LOCATIONS.
15. CONTRACTOR TO KEEP ALL ROADWAYS OUTSIDE OF PROJECT LIMITS CLEAN AND FREE OF DEBRIS.

ORDER OF SHEETS-SECTION 2

UTILITY CONTACT LIST

Dairyland Power

Jane Eggen
P.O. Box 817
La Crosse, WI 54602
608.788.4000
jane.eggen@dairylandpower.com

Charter Communications

Perry McClellan
1228 12th Ave S
Onalaska, WI 54650
715.370.7140
Perry.McClellan@chartercom.com

CenturyLink

Brian Stelplugh
333 N Front St
La Crosse, WI 54601
608.796.5543 / 608.796.5142(cell)
brian.stelplugh@centurylink.com

Xcel Energy (Electricity)

Scott Roberts
3215 Commerce Street
La Crosse, WI 54603
608.789.3625
Scott.w.roberts@xcelenergy.com

Xcel Energy (Gas)

Ed Przytarski
3215 Commerce Street
La Crosse, WI 54603
608.789.3631

Riverland Energy

Tim Holtan
N29988 State Road 93
Arcadia, WI 54612
608.797.0102 (cell)
608.323.3381 (office)
tholtan@riverlandenergy.com

DNR Liaison Contact

Karen Kalvelage
DNR Service Center
3550 Mormon Coulee Road
La Crosse, WI 54601
608.785.9115
karen.kalvelage@wisconsin.gov

City of Onalaska

Jarrod Holter, PE
City Engineer
608.781.9537
jholter@cityofonalaska.com

CONSULTANT DESIGN

JEREMY TOMESH
SEH INC.
312 SOUTH 3RD STREET
LA CROSSE, WI 54601
PHONE: (608) 498-4947
EMAIL: JTOMESH@SEHINC.COM

TO OBTAIN LOCATION OF PARTICIPANTS' UNDERGROUND FACILITIES BEFORE YOU DIG IN WISCONSIN

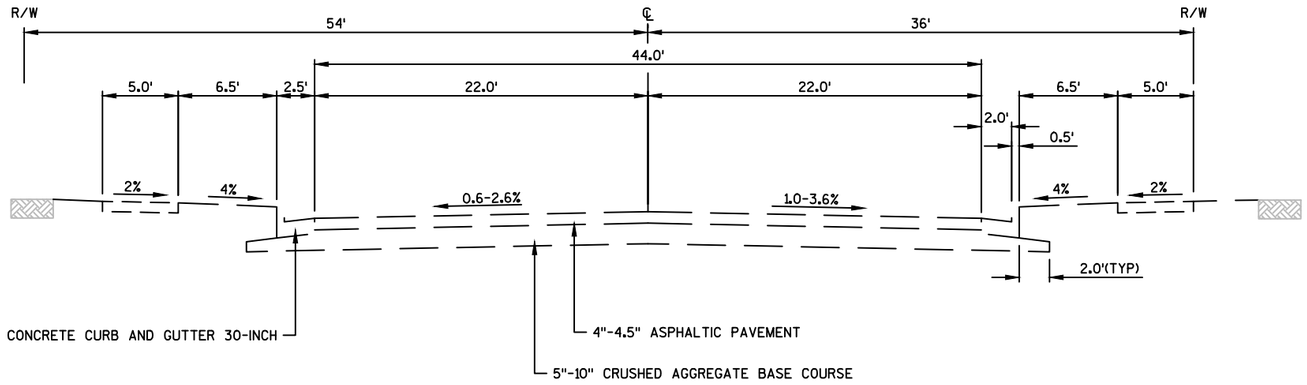
DIGGERS HOTLINE

Dial 811 or (800)242-8511

www.DiggersHotline.com

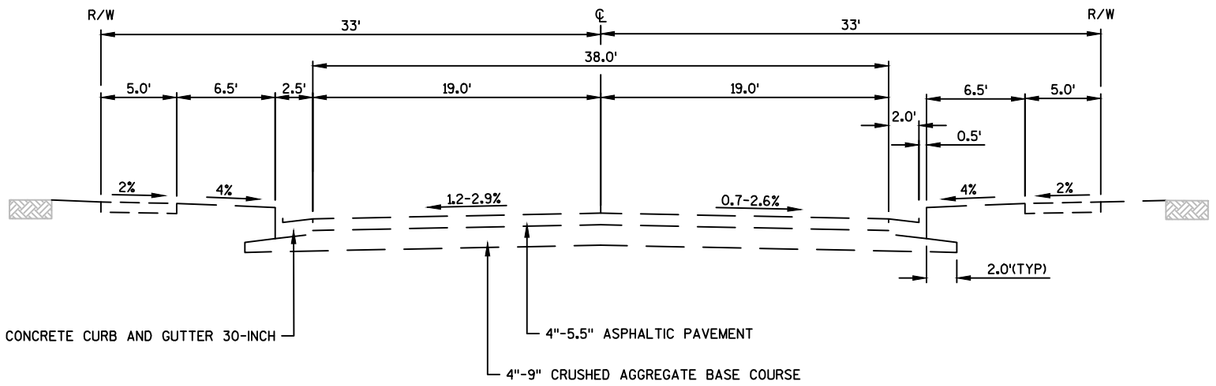
**DENOTES UTILITIES THAT ARE NOT DIGGERS HOTLINE MEMBERS

2



TYPICAL EXISTING SECTION

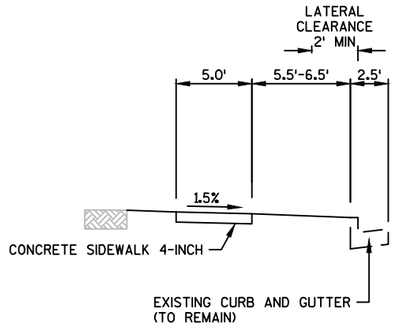
BRAUND STREET
STA 11+46.22 TO STA 20+86.19



TYPICAL EXISTING SECTION

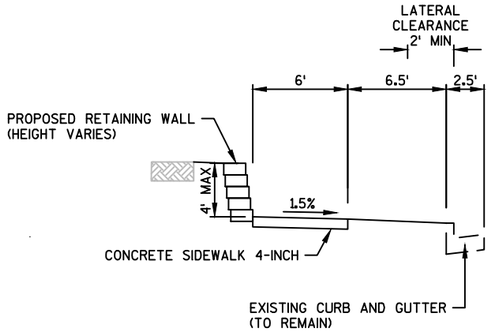
PH
STA 100+61.34 TO STA 112+00.00

PROJECT NO: 5591-02-54/55	HWY: BRAUND ST & PH	COUNTY: LA CROSSE	TYPICAL SECTIONS	SHEET 3	E



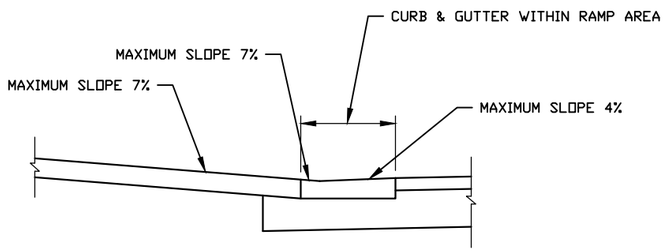
TYPICAL FINISHED PARTIAL SECTION

NEW SIDEWALK INSTALLATION

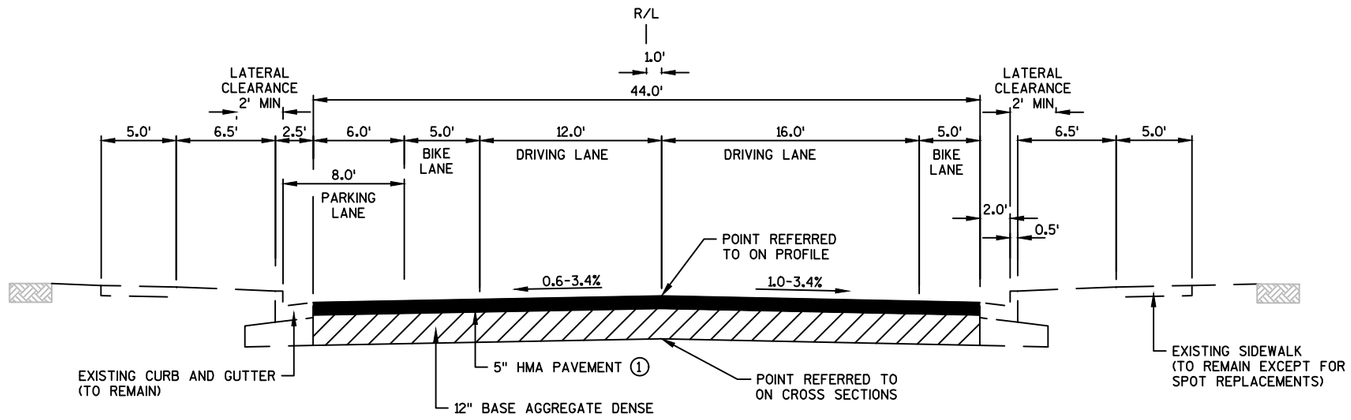


TYPICAL FINISHED PARTIAL SECTION

NEW SIDEWALK & RETAINING WALL
INSTALLATION



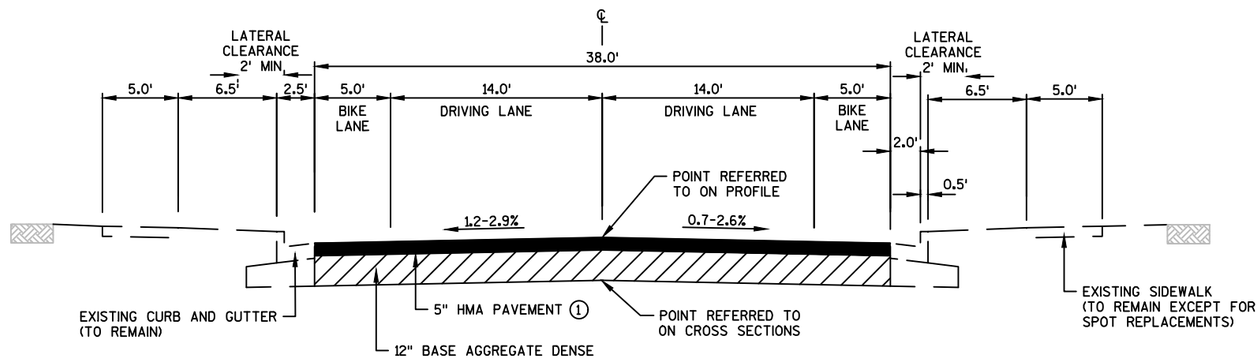
TYPICAL RAMP SECTION



TYPICAL FINISHED SECTION

BRAUND STREET
STA 11+46.22 TO STA 20+86.19

(1) 5" HMA PAVEMENT TO BE CONSTRUCTED IN 2 LAYERS
- 2.5" HMA PAVEMENT 4 LT 58-28 S UPPER LAYER
- 2.5" HMA PAVEMENT 3 LT 58-28 S LOWER LAYER



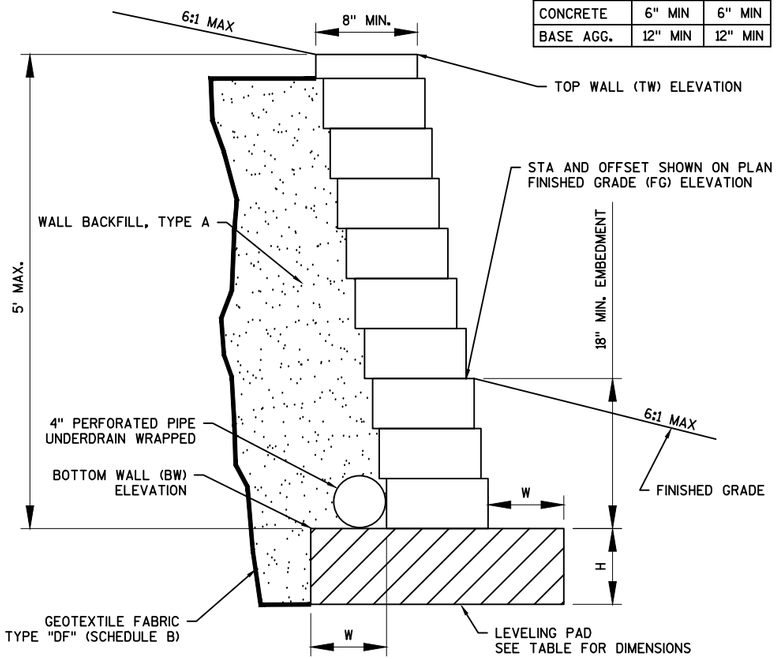
TYPICAL FINISHED SECTION

PH
STA 100+61.34 TO STA 112+00.00

PROJECT NO: 5591-02-54/55	HWY: BRAUND ST & PH	COUNTY: LA CROSSE	TYPICAL SECTIONS	SHEET 4	E

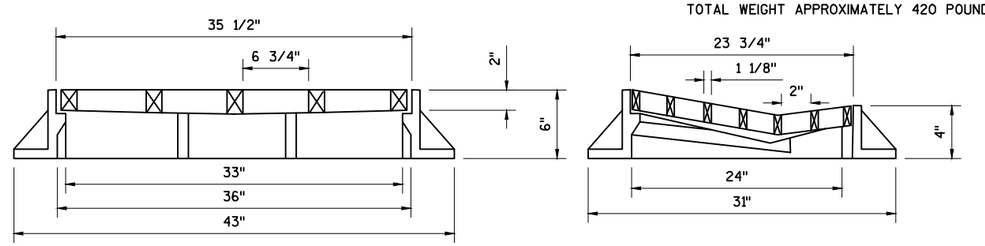
LEVELING PAD DIMENSIONS

MATERIAL	W	H
CONCRETE	6" MIN	6" MIN
BASE AGG.	12" MIN	12" MIN



TYPICAL WALL SECTION

TOTAL WEIGHT APPROXIMATELY 420 POUNDS



INLET COVERS TYPE DW

NOTES:
ALLOWABLE WALL SYSTEMS - WALL MODULAR BLOCK GRAVITY LANDSCAPE

THE PLAN QUANTITY FOR BID ITEM WALL MODULAR BLOCK GRAVITY LANDSCAPE IS BASED ON A WALL HEIGHT MEASURED FROM THE TOP OF WALL TO A CONSTANT DEPTH OF 1'-6" BELOW FINISHED GRADE. SEE MISCELLANEOUS QUANTITY SHEETS FOR PLAN QUANTITY.

THE CONTRACTOR SHALL PROVIDE COMPLETE DESIGN, PLANS, DETAILS, SPECIFICATIONS, AND SHOP DRAWINGS FOR THE RETAINING WALL IN ACCORDANCE WITH THE SPECIAL PROVISIONS. THE RETAINING WALL MANUFACTURER SHALL PROVIDE TECHNICAL ASSISTANCE TO THE CONTRACTOR DURING CONSTRUCTION. THE COST OF FURNISHING THESE ITEMS SHALL BE INCLUDED IN THE BID ITEM "WALL MODULAR BLOCK GRAVITY LANDSCAPE (STATION)".

PLANS, ELEVATIONS, AND DETAILS SHOWN ON THESE DRAWINGS ARE INTENDED TO INDICATE WALL LOCATIONS, LENGTHS, HEIGHTS, AND DETAILS COMMON TO THE WALL SYSTEM SELECTED. THE CONTRACTOR SHALL VERIFY THAT THE WALL SYSTEM SELECTED WILL CONFORM TO THE REQUIRED ALIGNMENTS AND DETAILS.

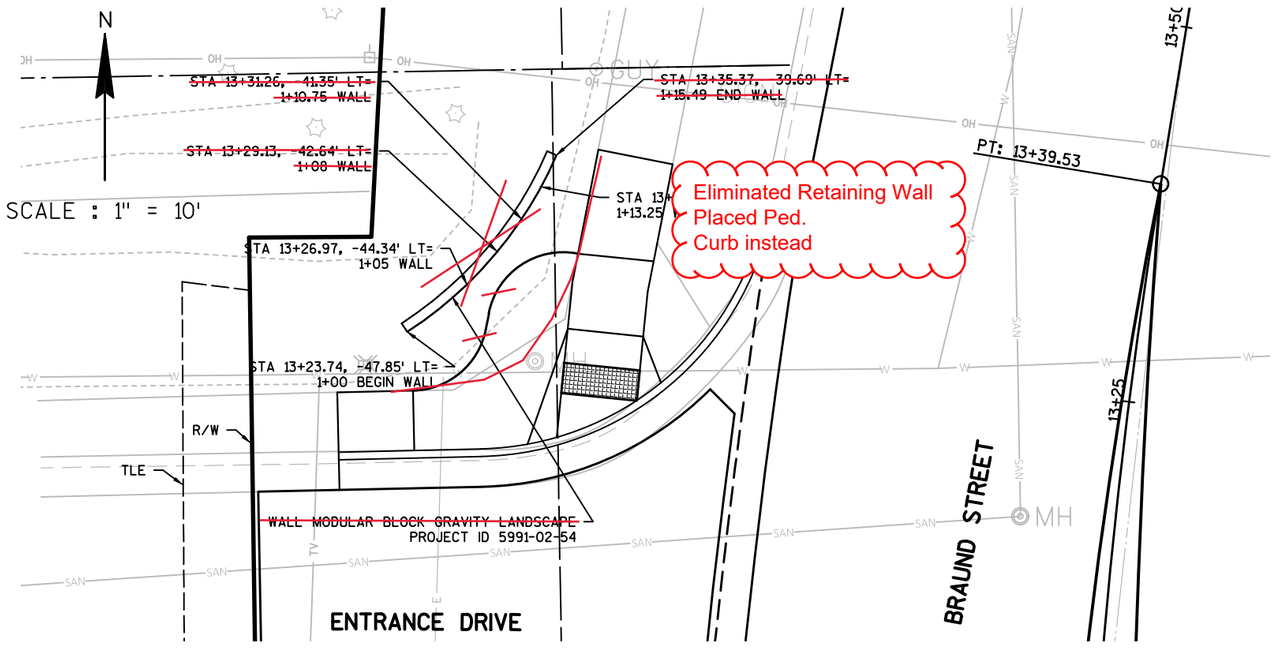
ALLOWABLE SOIL BEARING CAPACITY = 1000 PSF

ELEVATIONS AND LENGTHS ARE APPROXIMATE. CONSTRUCT TO FIT TERRAIN.

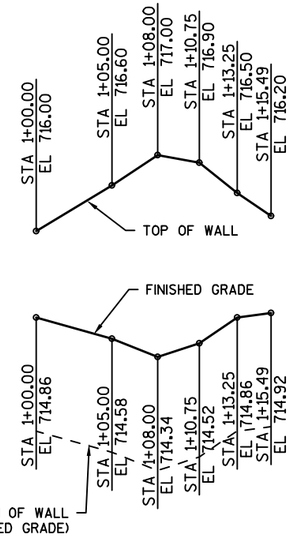
DESIGN FOR THE RETAINING WALL TO PROVIDE FOR THE FINISHED SLOPE BEHIND THE WALL AS SHOWN ON THE CROSS SECTIONS.

WALL TO HAVE SPLIT BLOCK APPEARANCE AND BE TAN IN COLORATION.

DRAWING NOT TO SCALE UNLESS INDICATED.

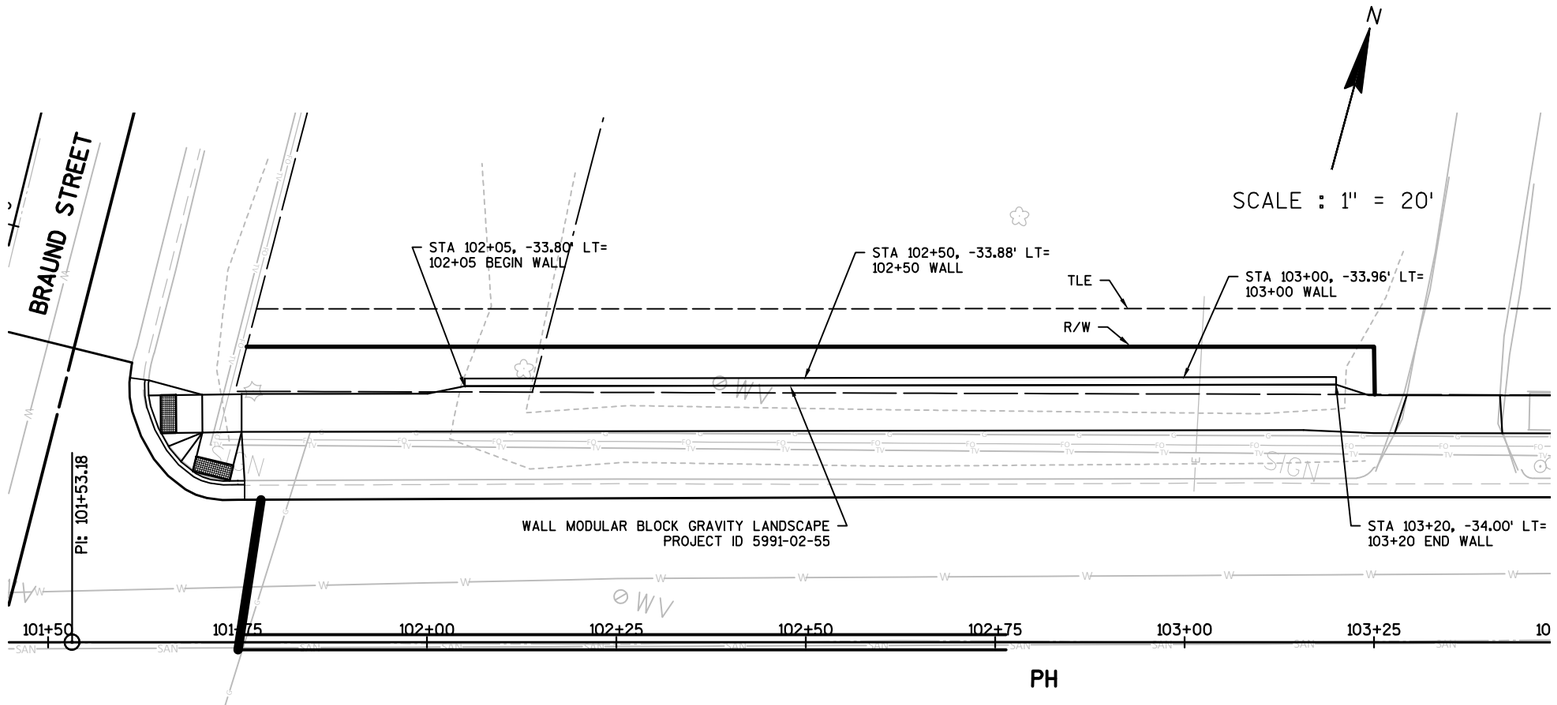


RETAINING WALL PLAN - BRAUND STREET

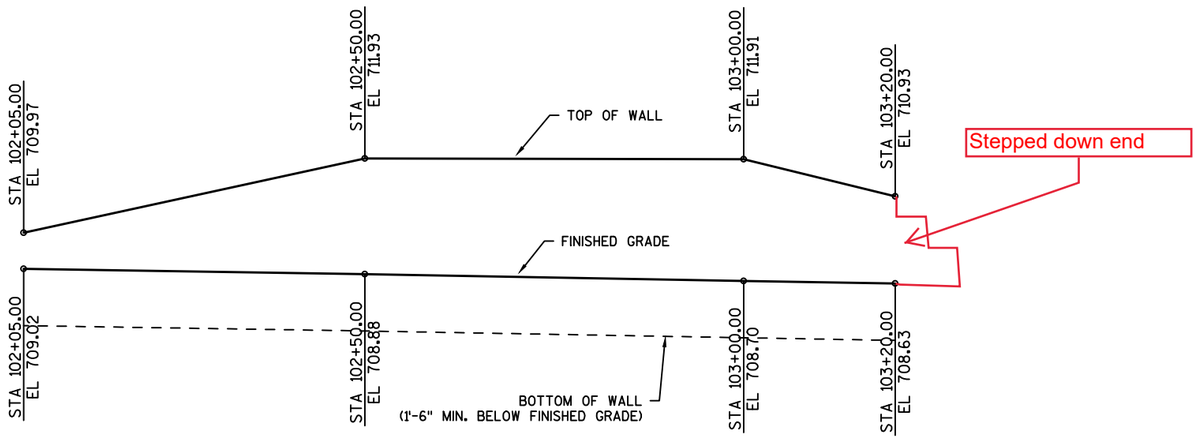


RETAINING WALL ELEVATION - BRAUND

PROJECT NO: 5991-02-54/55	HWY: BRAUND & PH	COUNTY: LA CROSSE	RETAINING WALL & INLET COVERS TYPE DW DETAILS	SHEET 5	E



RETAINING WALL PLAN - PH



RETAINING WALL ELEVATION - PH

NOTES:
ALLOWABLE WALL SYSTEMS - WALL MODULAR BLOCK GRAVITY LANDSCAPE

THE PLAN QUANTITY FOR BID ITEM WALL MODULAR BLOCK GRAVITY LANDSCAPE IS BASED ON A WALL HEIGHT MEASURED FROM THE TOP OF WALL TO A CONSTANT DEPTH OF 1'-6" BELOW FINISHED GRADE. SEE MISCELLANEOUS QUANTITY SHEETS FOR PLAN QUANTITY.

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ALLOWABLE SOIL BEARING CAPACITY = 1000 PSF

ELEVATIONS AND LENGTHS ARE APPROXIMATE. CONSTRUCT TO FIT TERRAIN.

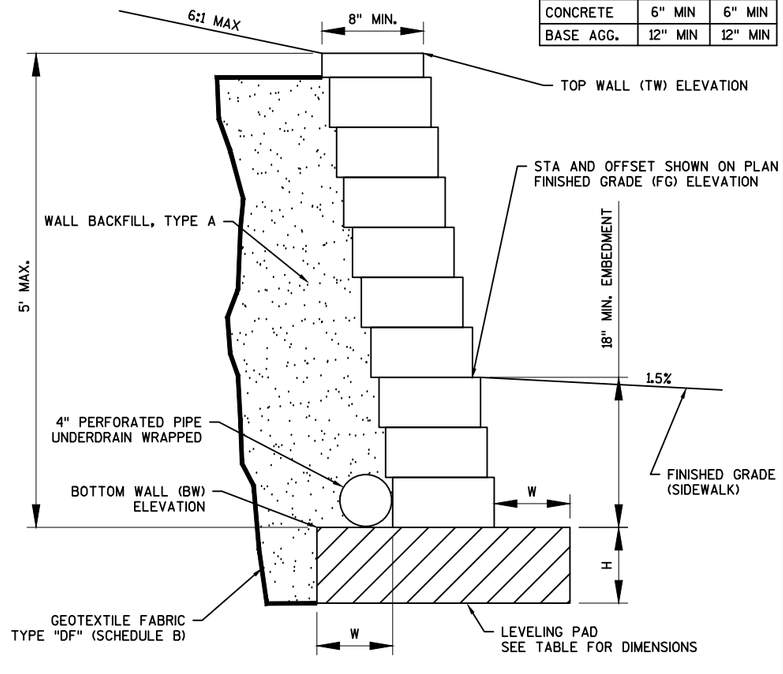
DESIGN FOR THE RETAINING WALL TO PROVIDE FOR THE FINISHED SLOPE BEHIND THE WALL AS SHOWN ON THE CROSS SECTIONS.

WALL TO HAVE SPLIT BLOCK APPEARANCE AND BE TAN IN COLORATION.

DRAWING NOT TO SCALE UNLESS INDICATED.

LEVELING PAD DIMENSIONS

MATERIAL	W	H
CONCRETE	6" MIN	6" MIN
BASE AGG.	12" MIN	12" MIN



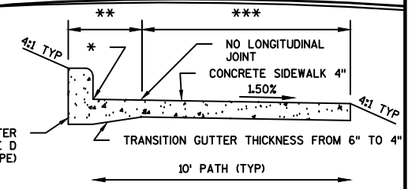
TYPICAL WALL SECTION

PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	RETAINING WALL DETAILS	SHEET 6	E

2

MATCH EXIST (710.57)
710.64
710.64
SLOPE INTERCEPT
MATCH EXIST (710.55)
EDGE OF EXISTING PARKING LOT ASPHALT
BEGIN CONCRETE CURB & GUTTER 30-INCH TYPE D (2' TRANSITION)
PROPOSED TLE TYPICAL
CONCRETE SIDEWALK 4"
EXISTING R/W TYPICAL
PROPOSED R/W TYPICAL
PROPOSED R/W TYPICAL
R=260'
712.54
712.44
711.64
711.24
710.82
R=100'
710.86
MULTI-USE PATH
711.88
712.29
711.49
R=250'
711.09
710.70
R=25'
710.71
R=110'
STH 16
N
SCALE : 1" = 10'
FLOW DIRECTION
BRAUND STREET

* POINT REFERRED TO BY SPOT ELEVATION
** PAY LIMITS OF CONCRETE C&G
*** PAY LIMITS OF CONCRETE SIDEWALK 4-INCH



PATH DETAIL WITH CONCRETE CURB & GUTTER

PROPOSED TLE TYPICAL
EDGE OF EXISTING PARKING LOT ASPHALT
PROPOSED R/W TYPICAL
MATCH EXIST (714.21)
714.68
714.29
714.51
714.33
MATCH EXIST (714.26)
714.76
714.95
714.39
ND CONCRETE CURB & GUTTER 30-INCH TYPE D (2' TRANSITION)
PROPOSED TLE TYPICAL
CONCRETE SIDEWALK 4"
R=25'
R=25'
713.55
713.09
713.05
713.13
713.41
713.24
1.84% GUTTER SLOPE AT RAMP
713.53
ENTRANCE DRIVE
Pedestrian curb
TW716.00
BW714.86
PROPOSED RETAINING WALL
Eliminated Wall
EXISTING R/W TYPICAL
TW716.60
TW717.00
BW714.34
R=5'
TW716.90
ADJUST CASTING T/CAST=714.30
TW716.50
BW714.86
TW716.20
BW714.92
714.65
714.70
R=5'
714.53
714.45
714.30
714.27
713.93
713.89
7.2'
714.19
714.25
7'
5'
713.97
EXISTING R/W TYPICAL
10'
MULTI-USE PATH
713.40
R=15'
712.86
712.82
TRUNCATED DOME
713.81
714.11
714.17
714.82
R=35'
712.59
2.5'
712.81
713.32
1.94% GUTTER SLOPE AT RAMP
714.21
6.5'
714.75
713.85
713.77
CONCRETE SIDEWALK 4"
713.10
713.97
712.90
712.75
713.00
713.60
713.53
713.57
SLOPE INTERCEPT
712.45
MATCH EXIST (712.42)
MATCH EXIST (713.60)
N
SCALE : 1" = 10'
BRAUND STREET

PROJECT NO:5991-02-54	HWY:BRAUND STREET	COUNTY:LA CROSSE	BRAUND STREET & ENTRANCE DRIVE INTERSECTION DETAILS	SHEET 7	E

SCALE : 1" = 10'

BRAUND STREET

PH

20+00

21+00

101+00

PI: 101+11.66

PI: 101+53.18

709.32 709.17 709.15 709.12 709.11 709.03 709.70 709.06 709.03 708.95 708.59 708.67 708.96 708.55 708.63 708.71 708.63 709.19 709.16 708.72 708.68 708.76 708.74 708.66

5' 5'

MATCH EXISTING (708.65)

TRUNCATED DOME

0.34% GUTTER SLOPE AT RAMP

CONCRETE SIDEWALK 4"

MATCH EXISTING (708.53)

1.60% GUTTER SLOPE AT RAMP

TRUNCATED DOME

MATCH EXISTING (708.40) REPLACE EXISTING CASTING WITH INLET COVER TYPE DW

708.79 708.40 708.40 708.38 708.34 708.42 708.73 708.40 708.30 708.32 708.36 708.28 708.22 708.34 708.48 708.38 708.30 708.47 708.56 708.64

5'

0.97% GUTTER SLOPE AT RAMP

TRUNCATED DOME

CONCRETE SIDEWALK 4"

PROPOSED TLE TYPICAL

EXISTING R/W TYPICAL

CONCRETE SIDEWALK 4" TYPICAL

707.86 707.76 708.37 707.82 707.90 707.72 707.80 708.20

MATCH EXISTING (707.70)

2.00% GUTTER SLOPE AT RAMP

TRUNCATED DOME

708.71

REMOVE EXISTING INLET
REMOVE STORM SEWER 12-INCH

CONNECT TO EXISTING MH
INV IN=701.84

TRUNCATED DOME

0.70% GUTTER SLOPE AT RAMP

SSPRC CLIII 12-INCH - 19 LF
S=17.8%

MATCH EXISTING (708.10)

708.06 707.99 708.03 707.99 707.95 708.02

INLET 2X3-FT
INLET COVER TYPE H-S
FL=707.92
INV OUT=705.22

708.07 708.49 708.03 707.99 708.06 708.46 708.38 708.45

0.50% GUTTER SLOPE AT RAMP

TRUNCATED DOME

5' 5'

708.54 708.46 708.11 708.09 708.02 708.06

CONCRETE SIDEWALK 4"

PROPOSED R/W TYPICAL

PROPOSED TLE TYPICAL

MATCH EXISTING (708.40)

0.18% GUTTER SLOPE AT RAMP

TRUNCATED DOME

708.21 708.14 708.22 708.13 708.65 708.68 708.23 708.19 708.27 708.61 708.17 708.18 708.64 708.56 708.72 708.84 708.64 708.38 708.30 708.34

MATCH EXISTING (708.11)

2.00% GUTTER SLOPE AT RAMP

TRUNCATED DOME

CONCRETE SIDEWALK 4" TYPICAL

PROPOSED R/W TYPICAL

PROPOSED TLE TYPICAL

MATCH EXISTING (708.34)

CONCRETE SIDEWALK 4"

PROJECT NO:5991-02-54	HWY:BRAUND STREET	COUNTY:LA CROSSE	BRAUND STREET & PH INTERSECTION DETAILS	SHEET 8	E

2

2

N

SCALE : 1" = 10'

LESTER AVENUE

PROPOSED TLE TYPICAL

EXISTING R/W TYPICAL

MATCH EXISTING (706.43)

706.93

706.48

706.44

706.52

707.08

TRUNCATED DOME

0.35% GUTTER SLOPE AT RAMP

7.2'

707.00

706.50

706.97

706.54

706.46

CONCRETE SIDEWALK 4"

MATCH EXISTING (706.47)

MATCH EXISTING (706.20) 1 FOOT CURB TRANSITION

706.26

706.30

706.70

706.22

6.8'

706.82

TRUNCATED DOME

1.07% GUTTER SLOPE AT RAMP

5'

706.32

706.28

706.79

706.79

706.36

CONCRETE SIDEWALK 4"

MATCH EXISTING (706.29)

PROPOSED TLE TYPICAL

PH

106+00

107+00

706.76

SIGN

PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	PH & LESTER AVENUE INTERSECTION DETAILS	SHEET 9	E

FILE NAME : \\SEHLX1\PROJECTS\K0\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\021101-ID-PH.DWG
LAYOUT NAME - 021101-ID-PH - 3

PLOT DATE : 6/1/2017 7:22 AM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:10 FT

2

1-00R 1-01 ONE WAY R6-2-L 24"X30"

1-06R 1-07 DIVIDED HIGHWAY R6-3 30"X24"

1-10R 1-11 ONLY R3-8-A 36"X30"

1-14R 1-15 STOP R1-1 30"X30"

ENTRANCE DRIVE

1-16R 1-17 ONLY R3-8-A 36"X30"

1-18 ENDS BIKE LANE R3-17-BP 30"X12" R3-17 30"X24"

STOP R1-1F 36"X36"

11+00 12+00 13+00 14+00 15+00 16+00

BRAUND STREET

MATCH LINE STA 16+50.00

1-02R 1-03 R4-7 24"X30"

1-04R 1-05 STOP R1-1F

1-08R 1-09 R4-7 24"X30"

BIKE LANE R3-17 30"X24" 1-12

1-13 NO PARKING ON THIS SIDE R7-53D 18"X24"

1-19R 1-20 SPEED LIMIT 25 R2-1 24"X30"

1-21R 1-22 NO STOPPING OR STANDING NO PARKING ON THIS SIDE R7-53D 18"X24"

*ATTACH SIGN TO EXISTING TRAFFIC SIGNAL POST

PAVEMENT MARKING LEGEND

- (A) MARKING LINE EPOXY 4-INCH (WHITE)
- (B) MARKING LINE EPOXY 4-INCH (YELLOW, 12.5' SKIP)
- (C) MARKING LINE EPOXY 4-INCH (YELLOW)
- (D) MARKING LINE EPOXY 8-INCH (WHITE)
- (E) MARKING STOP LINE EPOXY 18-INCH (WHITE)
- (F) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (G) MARKING SYMBOL EPOXY (BIKE LANE) AND MARKING ARROW EPOXY (BIKE LANE)
- (H) MARKING SYMBOL EPOXY (BIKE LANE SHARED)
- (I) MARKING ARROW EPOXY (LEFT)
- (J) MARKING ARROW EPOXY (THRU-LEFT)
- (K) MARKING WORD EPOXY
- (L) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (M) MARKING LINE EPOXY 4-INCH (WHITE, 3' SKIP)
- (N) MARKING LINE EPOXY 8-INCH (WHITE, 3' SKIP)

2-03 NO PARKING HERE TO CORNER R7-51R 18"X24"

2-06 BIKE LANE R3-17 30"X24"

STOP R1-1 36"X36" ALL WAY R1-3P 18"X6" 2-09R 2-10

MATCH LINE STA 16+50.00

17+00 18+00 19+00 20+00 21+00 21+52

BRAUND STREET

12' 12' 5' 11'

2-00R 2-01 NO PARKING ON THIS SIDE R7-53D 18"X24"

Sign 2-01Moved 25' due to Right Turn Lane

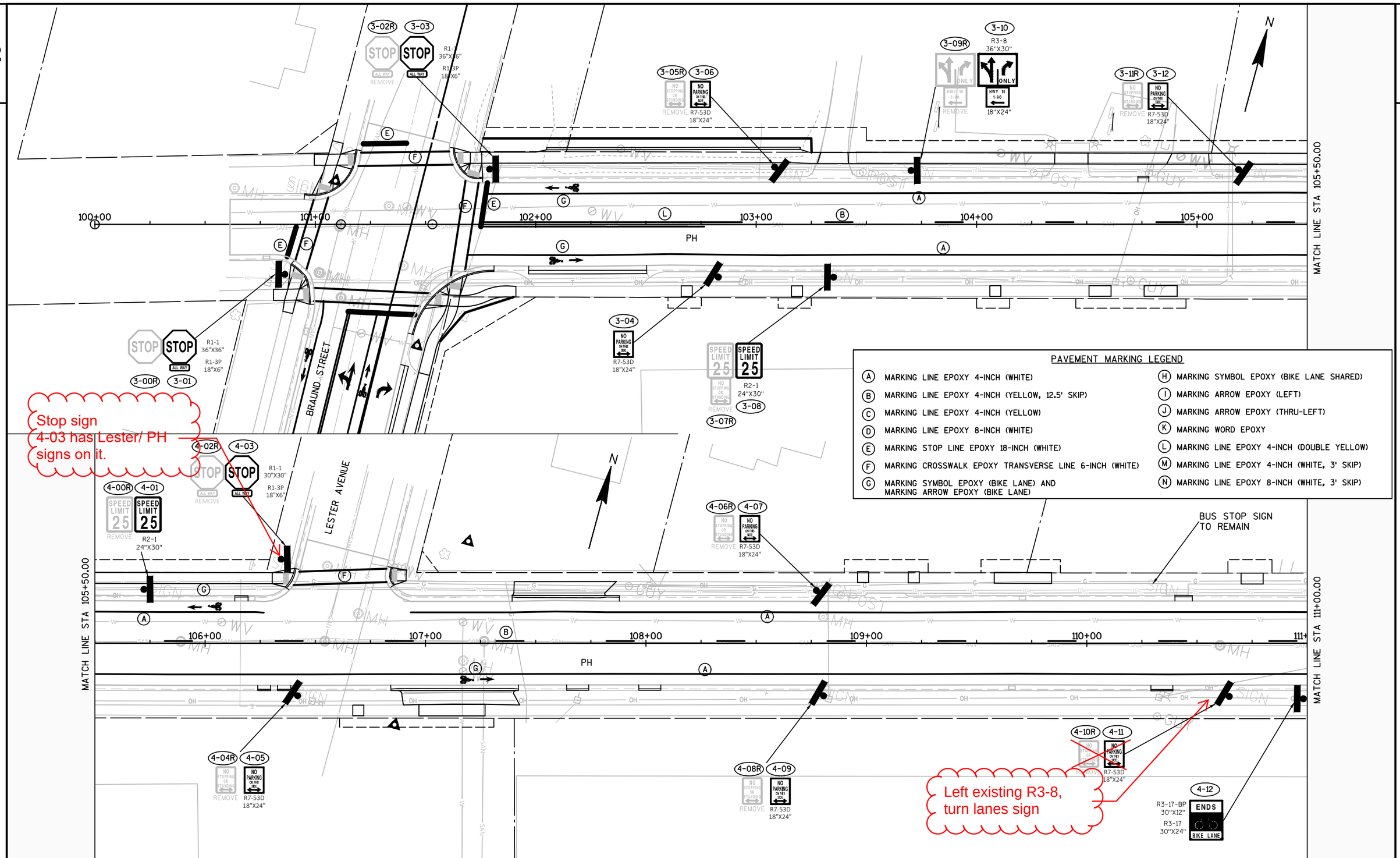
BEGIN RIGHT TURN LANE YIELD TO BIKES R4-4 36"X30" 2-02

2-04R 2-05 ONLY R3-8 36"X30"

STOP R1-1 36"X36" ALL WAY R1-3P 18"X6" 2-07R 2-08

PH

PROJECT NO: 5991-02-54	HWY: BRAUND STREET	COUNTY: LA CROSSE	PAVEMENT MARKING & SIGNING BRAUND STREET	SHEET 10	E



PAVEMENT MARKING LEGEND

- (A) MARKING LINE EPOXY 4-INCH (WHITE)
- (B) MARKING LINE EPOXY 4-INCH (YELLOW, 12.5' SKIP)
- (C) MARKING LINE EPOXY 4-INCH (YELLOW)
- (D) MARKING LINE EPOXY 8-INCH (WHITE)
- (E) MARKING STOP LINE EPOXY 18-INCH (WHITE)
- (F) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (G) MARKING SYMBOL EPOXY (BIKE LANE) AND MARKING ARROW EPOXY (BIKE LANE)
- (H) MARKING SYMBOL EPOXY (BIKE LANE SHARED)
- (I) MARKING ARROW EPOXY (LEFT)
- (J) MARKING ARROW EPOXY (THRU-LEFT)
- (K) MARKING WORD EPOXY
- (L) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (M) MARKING LINE EPOXY 4-INCH (WHITE, 3' SKIP)
- (N) MARKING LINE EPOXY 8-INCH (WHITE, 3' SKIP)

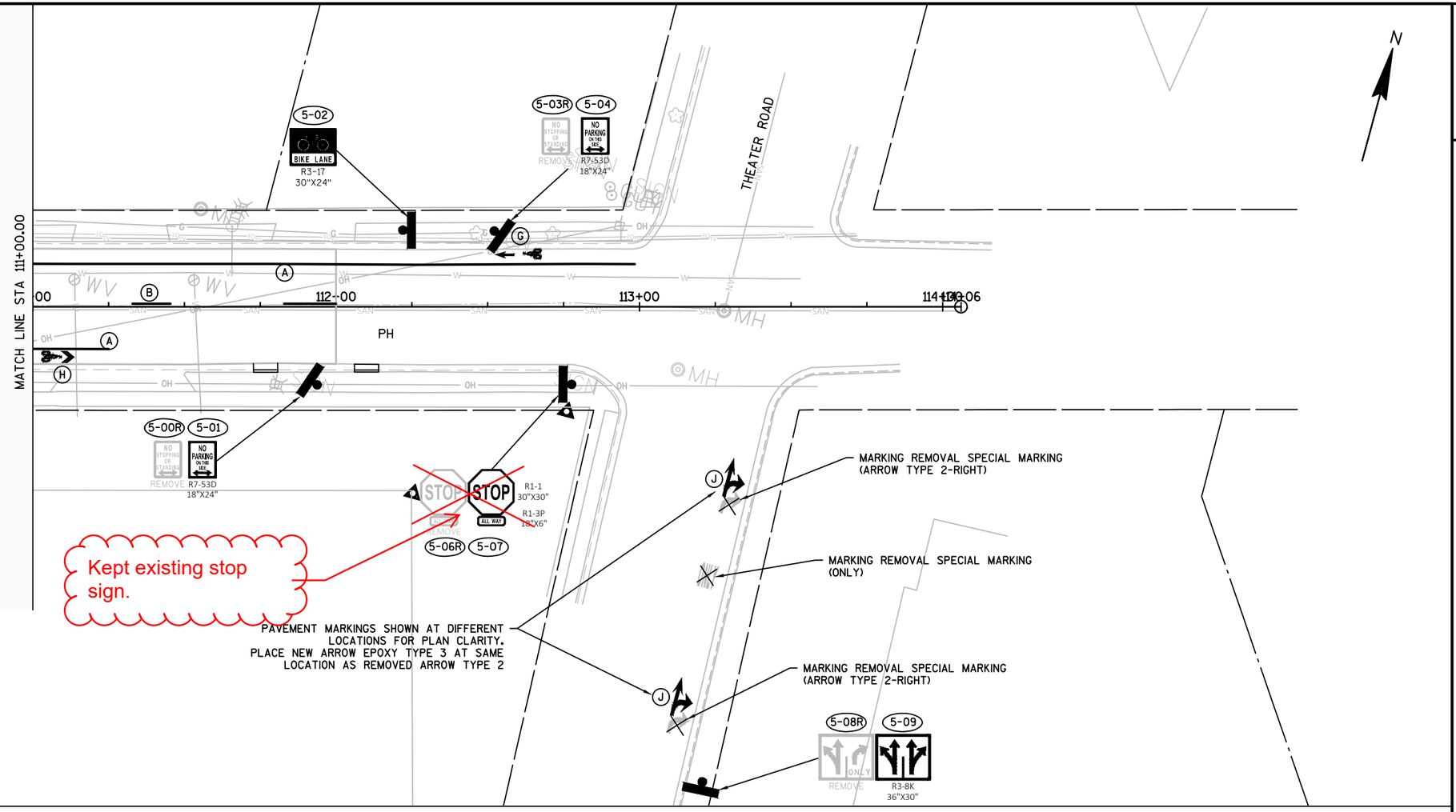
PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	PAVEMENT MARKING & SIGNING PH	SHEET 11	E

FILE NAME : \\SEHLX1\PROJECTS\K0\0\0NALA\133149\C3D\SHEETSPLAN\BRAUND\024501-PM-PH.DWG
LAYOUT NAME - 024501-PM-PH - PM 1

PLOT DATE : 6/12/2017 11:40 AM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:40 FT

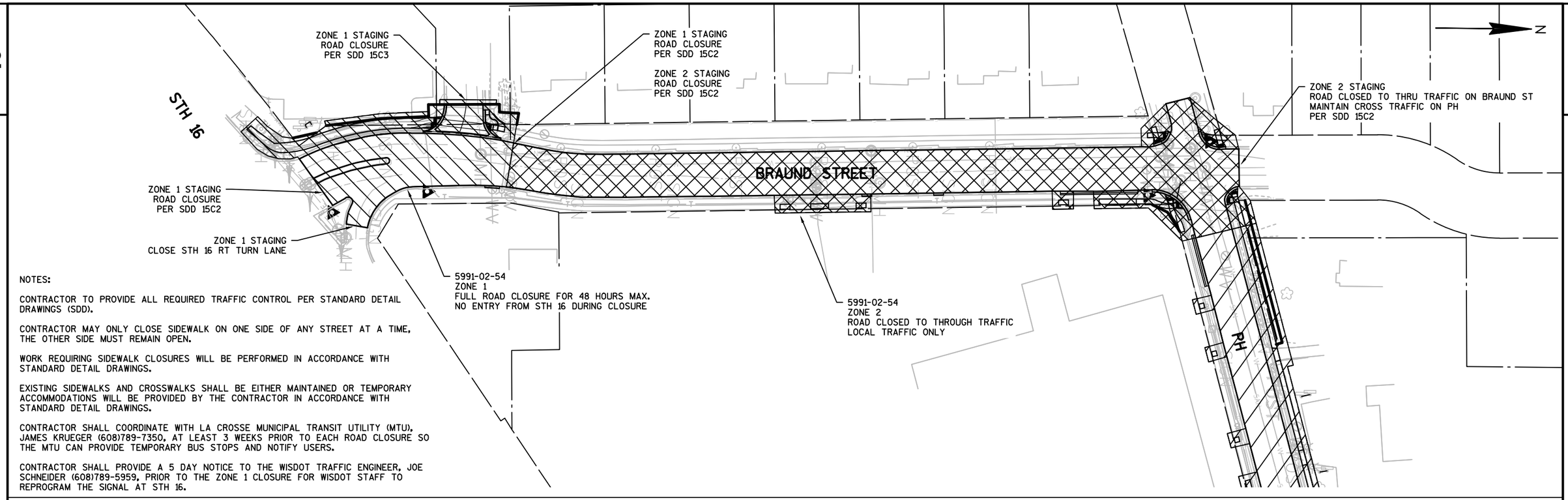
WISDOT/CADDS SHEET 42

2



PAVEMENT MARKING LEGEND

- (A) MARKING LINE EPOXY 4-INCH (WHITE)
- (B) MARKING LINE EPOXY 4-INCH (YELLOW, 12.5' SKIP)
- (C) MARKING LINE EPOXY 4-INCH (YELLOW)
- (D) MARKING LINE EPOXY 8-INCH (WHITE)
- (E) MARKING STOP LINE EPOXY 18-INCH (WHITE)
- (F) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (G) MARKING SYMBOL EPOXY (BIKE LANE) AND MARKING ARROW EPOXY (BIKE LANE)
- (H) MARKING SYMBOL EPOXY (BIKE LANE SHARED)
- (I) MARKING ARROW EPOXY (LEFT)
- (J) MARKING ARROW EPOXY (THRU-LEFT)
- (K) MARKING WORD EPOXY
- (L) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (M) MARKING LINE EPOXY 4-INCH (WHITE, 3' SKIP)
- (N) MARKING LINE EPOXY 8-INCH (WHITE, 3' SKIP)



NOTES:

CONTRACTOR TO PROVIDE ALL REQUIRED TRAFFIC CONTROL PER STANDARD DETAIL DRAWINGS (SDD).

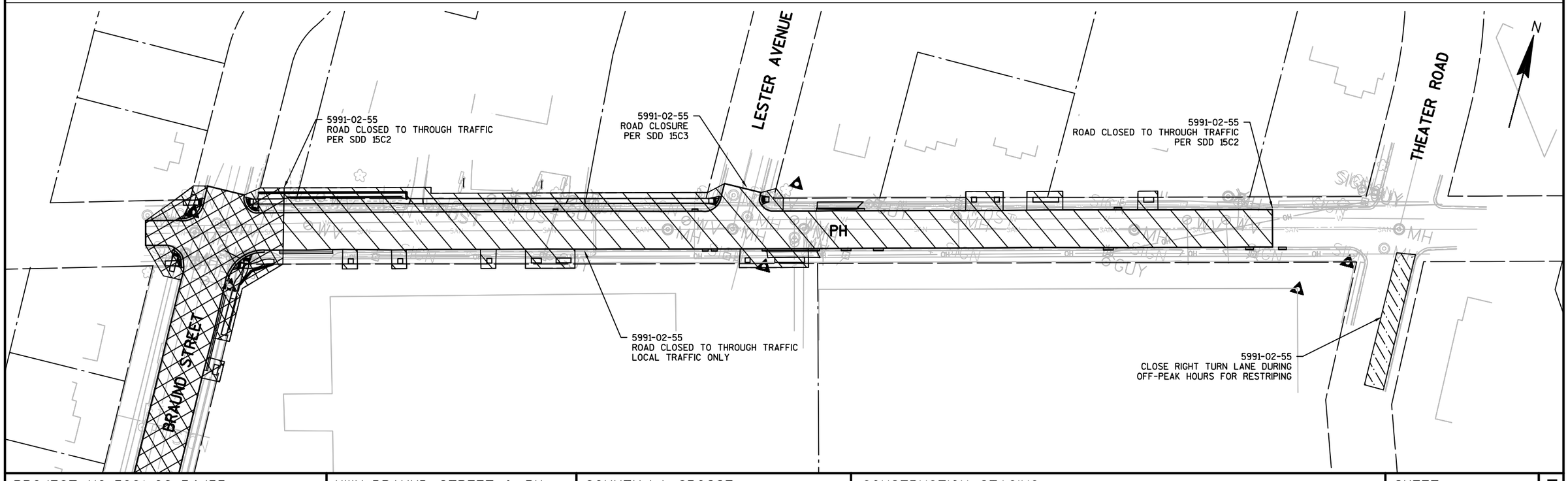
CONTRACTOR MAY ONLY CLOSE SIDEWALK ON ONE SIDE OF ANY STREET AT A TIME, THE OTHER SIDE MUST REMAIN OPEN.

WORK REQUIRING SIDEWALK CLOSURES WILL BE PERFORMED IN ACCORDANCE WITH STANDARD DETAIL DRAWINGS.

EXISTING SIDEWALKS AND CROSSWALKS SHALL BE EITHER MAINTAINED OR TEMPORARY ACCOMMODATIONS WILL BE PROVIDED BY THE CONTRACTOR IN ACCORDANCE WITH STANDARD DETAIL DRAWINGS.

CONTRACTOR SHALL COORDINATE WITH LA CROSSE MUNICIPAL TRANSIT UTILITY (MTU), JAMES KRUEGER (608)789-7350, AT LEAST 3 WEEKS PRIOR TO EACH ROAD CLOSURE SO THE MTU CAN PROVIDE TEMPORARY BUS STOPS AND NOTIFY USERS.

CONTRACTOR SHALL PROVIDE A 5 DAY NOTICE TO THE WISDOT TRAFFIC ENGINEER, JOE SCHNEIDER (608)789-5959, PRIOR TO THE ZONE 1 CLOSURE FOR WISDOT STAFF TO REPROGRAM THE SIGNAL AT STH 16.



PROJECT NO: 5991-02-54/55	HWY: BRAUND STREET & PH	COUNTY: LA CROSSE	CONSTRUCTION STAGING	SHEET 13	E

FILE NAME : \\SEHLX1\PROJECTS\KO\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\026001-S1.DWG
LAYOUT NAME - 026001-S1 - 1

PLOT DATE : 7/25/2017 8:24 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42

Estimate Of Quantities By Plan Sets

3

					5991-02-54	5991-02-55	
Line	**Item**	**Item Description**	**Unit**	**Total**	**Qty**	**Qty**	
0006	204.0100	Removing Pavement	SY	70.000		70.000	
0008	204.0150	Removing Curb & Gutter	LF	589.000	330.000	259.000	
0010	204.0155	Removing Concrete Sidewalk	SY	693.000	365.000	328.000	
0012	204.0220	Removing Inlets	EACH	1.000	1.000		
0014	204.0245	Removing Storm Sewer (size) 01. 12-Inch	LF	19.000	19.000		
0016	205.0100	Excavation Common	CY	5,090.000	2,623.000	2,467.000	
0018	213.0100	Finishing Roadway (project) 01. 5991-02-54	EACH	1.000	1.000		
0020	213.0100	Finishing Roadway (project) 01. 5991-02-55	EACH	1.000		1.000	
0024	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	6,458.000	3,513.000	2,945.000	
0026	312.0110	Select Crushed Material	TON	760.000		760.000	
0028	416.0160	Concrete Driveway 6-Inch	SY	154.000	40.000	114.000	
0030	455.0605	Tack Coat	GAL	534.000	291.000	243.000	
0032	460.2000	Incentive Density HMA Pavement	DOL	1,760.000	960.000	800.000	
0034	460.5223	HMA Pavement 3 LT 58-28 S	TON	1630.00	7887	6743	CCO 1
0036	460.6224	HMA Pavement MT 58-28 S	TON	1086.00	7359	495	CCO 1#3
0038	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	570.000	311.000	259.000	
0040	602.0405	Concrete Sidewalk 4-Inch	SF	5,432.000	2,790.000	2,642.000	
0042	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	132.000	112.000	20.000	
0046	608.0312	Storm Sewer Pipe Reinforced Concrete Class III 12-Inch	LF	19.000	19.000		
0048	611.0639	Inlet Covers Type H-S	EACH	1.000	1.000		
0050	611.3230	Inlets 2x3-FT	EACH	1.000	1.000		
0052	611.8110	Adjusting Manhole Covers	EACH	16.000	9.000	7.000	
0058	619.1000	Mobilization	EACH	0.400	0.200	0.200	
0060	624.0100	Water	MGAL	193.800	105.400	88.400	
0062	625.0100	Topsoil	SY	301.000	128.000	173.000	
0064	627.0200	Mulching	SY	301.000	128.000	173.000	
0066	628.1905	Mobilizations Erosion Control	EACH	4.000	2.000	2.000	
0068	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	2.000	2.000	
0070	628.7015	Inlet Protection Type C	EACH	9.000	3.000	6.000	
0072	629.0210	Fertilizer Type B	CWT	0.420	0.170	0.250	
0074	630.0140	Seeding Mixture No. 40	LB	5.420	2.310	3.110	
0076	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	37.000	18.000	19.000	
0080	637.2210	Signs Type II Reflective H	SF	194.670	110.100	84.570	
0082	637.2215	Signs Type II Reflective H Folding	SF	14.920	14.920		
0084	638.2602	Removing Signs Type II	EACH	30.000	15.000	15.000	
0086	638.3000	Removing Small Sign Supports	EACH	27.000	12.000	15.000	
0088	642.5001	Field Office Type B	EACH	0.500	0.250	0.250	
0090	643.0300	Traffic Control Drums	DAY	608.000	20.000	588.000	
0092	643.0410	Traffic Control Barricades Type II	DAY	236.000	124.000	112.000	
225	201.0120	Clearing	ID	47.00		47.00	
230	201.0220	Grubbing	ID	47.00		47.00	
205	601.0600	Concrete Curb Pedestrian	LF	100.00	100.00		
280	611.8115	Adjusting Inlet Covers	EACH	2.0	1.0	1.0	

					5991-02-54	5991-02-55
Line	**Item**	**Item Description**	**Unit**	**Total**	**Qty**	**Qty**
0094	643.0420	Traffic Control Barricades Type III	DAY	1,586.000	1,026.000	560.000
0096	643.0705	Traffic Control Warning Lights Type A	DAY	4,664.000	2,788.000	1,876.000
0098	643.0900	Traffic Control Signs	DAY	1,518.000	874.000	644.000
0104	643.5000	Traffic Control	EACH	0.400	0.200	0.200
0106	644.1420.S	Temporary Pedestrian Surface Plywood	SF	608.000	456.000	152.000
0108	644.1601.S	Temporary Curb Ramp	EACH	8.000	6.000	2.000
0110	646.1020	Marking Line Epoxy 4-Inch	LF	5,177.000	2,724.500	2,452.500
0112	646.3020	Marking Line Epoxy 8-Inch	LF	264.000	264.000	
0114	646.5020	Marking Arrow Epoxy	EACH	20.000	13.000	7.000
0116	646.5120	Marking Word Epoxy	EACH	2.000	2.000	
0118	646.5220	Marking Symbol Epoxy	EACH	11.000	5.000	6.000
0120	646.6120	Marking Stop Line Epoxy 18-Inch	LF	117.000	79.000	38.000
0124	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	505.500	423.500	82.000
0128	646.9300	Marking Removal Special Marking	EACH	3.000		3.000
0134	650.4000	Construction Staking Storm Sewer	EACH	1.000	1.000	
0136	650.4500	Construction Staking Subgrade	LF	2,079.000	940.000	1,139.000
0138	650.5000	Construction Staking Base	LF	2,079.000	940.000	1,139.000
0140	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	281.000	262.000	19.000
0142	650.9000	Construction Staking Curb Ramps	EACH	12.000	10.000	2.000
0144	650.9910	Construction Staking Supplemental Control (project) 01. 5991-02-54	LS	1.000	1.000	
0146	650.9910	Construction Staking Supplemental Control (project) 01. 5991-02-55	LS	1.000		1.000
0150	652.0800	Conduit Loop Detector	LF	55.000	55.000	
0152	655.0800	Loop Detector Wire	LF	65.000	65.000	
0154	690.0150	Sawing Asphalt	LF	502.000	422.000	80.000
0156	690.0250	Sawing Concrete	LF	422.000	194.000	228.000
0162	SPV.0060	Special 01. Existing Valve Box Adjustment	EACH	10.000	5.000	5.000
0164	SPV.0060	Special 02. Inlet Covers Type DW	EACH	1.000		1.000
0166	SPV.0090	Special 01. Concrete Curb & Gutter 30-Inch Type D SHES	LF	75.000	75.000	
0168	SPV.0090	Special 02. Construction Staking Multi-Use Path	LF	348.000	348.000	
0170	SPV.0165	Special 01. Wall Modular Block Gravity Landscape	SF	520.000	44.000	476.000
265	634.0616	POSTS WOOD 4X6X16-FT	EACH	3.0	1.0	2.0
210	804.2005	DISINCENTIVE DENSITY HMA PAVEMENT	DOLLAR	-3888.39	-761.34	-3127.05
200	SPV.0195	SPECIAL.01 EXTRA COST FOR ASPHALT MIX UPGRADE	TON	1086.00	591.00	495.00

REMOVING PAVEMENT

STATION	TO	STATION	DIR	LOCATION	204.0100 REMOVING PAVEMENT SY
PROJECT 5991-00-55 CATEGORY 0010					
106+89	-	107+42	RT	PH	39
107+40	-	107+86	LT	PH	31
				PROJECT TOTAL	70

REMOVING CURB AND GUTTER

STATION	DIR	LOCATION	204.0150 REMOVING CURB AND GUTTER LF
PROJECT 5991-00-54 CATEGORY 0010			
13+00	LT	BRAUND	75
13+25	RT	BRAUND	15
16+21	RT	BRAUND	10
17+04	RT	BRAUND	10
17+77	RT	BRAUND	10
19+05	RT	BRAUND	79
20+00	LT	BRAUND	22
20+00	RT	BRAUND	51
20+50	LT	BRAUND	36
20+50	RT	BRAUND	22
PROJECT 5991-00-55 CATEGORY 0010			
101+97	RT	PH	53
104+73	LT	PH	5
106+13	LT	PH	6
106+24	RT	PH	15
106+50	LT	PH/LESTER	19
106+84	RT	PH	58
107+39	RT	PH	49
107+64	RT	PH	10
107+97	RT	PH	10
110+29	RT	PH	10
110+40	LT	PH	8
111+73	RT	PH	8
112+06	RT	PH	8
		PROJECT TOTAL	589

REMOVING CONCRETE SIDEWALK

STATION	TO	STATION	DIR	LOCATION	204.0155 REMOVING SIDEWALK SY
PROJECT 5991-00-54 CATEGORY 0010					
11+46.22	-	12+75	LT	BRAUND	212
13+20	-	13+35	LT	BRAUND	14
16+21	-	16+31	RT	BRAUND	6
16+52	-	16+70	RT	BRAUND	10
16+98	-	17+08	RT	BRAUND	6
19+03	-	19+13	RT	BRAUND	6
19+44	-	20+20	RT	BRAUND	54
19+95	-	20+20	LT	BRAUND	20
20+50	-	20+75	LT	BRAUND	24
20+50	-	20+75	RT	BRAUND	13
PROJECT 5991-00-55 CATEGORY 0010					
101+60	-	106+40	LT	PH	251
102+66	-	102+71	RT	PH	3
103+16	-	103+21	RT	PH	3
104+06	-	104+11	RT	PH	3
104+51	-	104+61	RT	PH	6
104+76	-	104+91	RT	PH	9
106+67	-	106+72	RT	PH	3
106+85	-	106+95	LT	PH	5
106+97	-	107+27	RT	PH	18
108+96	-	109+01	LT	PH	3
109+19	-	109+24	LT	PH	3
109+58	-	109+84	LT	PH	15
111+70	-	111+80	LT	PH	6
				PROJECT TOTALS	693

EXCAVATION COMMON

STATION	TO	STATION	LOCATION	205.0100 EXCAVATION COMMON CY
PROJECT 5991-00-54 CATEGORY 0010				
11+46	-	13+50	BRAUND	773
13+50	-	20+86.19	BRAUND	1850
PROJECT 5991-00-55 CATEGORY 0010				
101+78	-	112+00	PH	2044
108+50	-	111+50	PH, EBS	423
			PROJECT TOTALS	5090

EXCAVATION COMMON INCLUDES EXISTING PAVEMENT.

3

FILE NAME : \\SEHLX1\PROJECTS\K0\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\030201_MQ.DWG
LAYOUT NAME - 030201_MQ
PLOT DATE : 11/3/2017 9:05 AM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : **********
WISDOT/CADDS SHEET 42

3

3

REMOVING INLETS

STATION	DIR	LOCATION	204.0220 REMOVING INLETS EACH
PROJECT 5991-00-54 CATEGORY 0010			
22+00	RT	BRAUND	1
		PROJECT TOTALS	1

CONCRETE DRIVEWAYS

STATION	TO	STATION	DIR	LOCATION	416.0160 6-INCH SY
PROJECT 5991-00-54 CATEGORY 0010					
16+52	-	16+70	RT	BRAUND	11
19+03	-	19+13	RT	BRAUND	6
19+44	-	19+82	RT	BRAUND	23
PROJECT 5991-00-55 CATEGORY 0010					
103+27	-	103+42	LT	PH	8
104+35	-	104+51	LT	PH	9
104+86	-	105+05	LT	PH	11
106+89	-	107+42	RT	PH	55
107+40	-	107+86	LT	PH	31
				PROJECT TOTAL	154

CONCRETE CURB AND GUTTER

STATION	DIR	LOCATION	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D LF	SPV.0090.01 CONCRETE CURB & GUTTER 30-INCH TYPE D SHES LF
PROJECT 5991-00-54 CATEGORY 0010				
12+00	LT	BRAUND	56	
13+00	LT	BRAUND		75
13+25	RT	BRAUND	15	
16+21	RT	BRAUND	10	
17+04	RT	BRAUND	10	
17+77	RT	BRAUND	10	
19+05	RT	BRAUND	79	
20+00	LT	BRAUND	22	
20+00	RT	BRAUND	51	
20+50	LT	BRAUND	36	
20+50	RT	BRAUND	22	
PROJECT 5991-00-55 CATEGORY 0010				
101+97	RT	PH	53	
104+73	LT	PH	5	
106+13	LT	PH	6	
106+24	RT	PH	15	
106+50	LT	PH/LESTER	19	
106+84	RT	PH	58	
107+39	RT	PH	49	
107+64	RT	PH	10	
107+97	RT	PH	10	
110+29	RT	PH	10	
110+40	LT	PH	8	
111+73	RT	PH	8	
112+06	RT	PH	8	
		PROJECT TOTALS	570	75

REMOVING STORM SEWER (SIZE)

STATION	TO	STATION	DIR	LOCATION	204.0245 12-INCH LF
PROJECT 5991-00-54 CATEGORY 0010					
20+20	-	20+27	RT	BRAUND	19
				PROJECT TOTAL	19

ASPHALTIC ITEMS

STATION	TO	STATION	LOCATION	460.2000 INCENTIVE DENSITY HMA PAVEMENT DOL	455.0605 TACK COAT GAL	460.5223 HMA PAVEMENT 3 LT 58-28 S TON	460.5224 HMA PAVEMENT 4 LT 58-28 S TON
PROJECT 5991-00-54 CATEGORY 0010							
11+46.22	-	13+50		260	78	198	198
13+50	-	20+86.19		700	213	541	541
PROJECT 5991-00-55 CATEGORY 0010							
101+78	-	112+00		800	243	619	619
			PROJECT TOTALS	1760	534	1358	1358

BASE, SUBGRADE AGGREGATE, AND WATER

STATION	TO	STATION	LOCATION	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	312.0110 SELECT CRUSHED MATERIAL TON*	624.0100 WATER** MGAL
PROJECT 5991-00-54 CATEGORY 0010						
11+46	-	13+50	BRAUND	940		28.2
13+50	-	20+86.19	BRAUND	2573		77.2
PROJECT 5991-00-55 CATEGORY 0010						
101+78	-	112+00	PH	2945	760	88.4
			PROJECT TOTALS	6458	760	193.7

*FOR AREAS OF EBS, SEE EARTHWORK TABLE FOR LOCATIONS.
SELECT CRUSHED MATERIAL ESTIMATED AT 1.8 TON/CY.
**INCLUDES WATER FOR DUST CONTROL

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CONCRETE SIDEWALK

STATION	TO	STATION	DIR	LOCATION	602.0405 CONCRETE SIDEWALK 4-INCH SF
PROJECT 5991-00-54 CATEGORY 0010					
11+46.22	-	12+75	LT	BRAUND	1790
13+20	-	13+35	LT	BRAUND	118
16+21	-	16+31	RT	BRAUND	50
16+98	-	17+08	RT	BRAUND	48
19+82	-	20+20	RT	BRAUND	284
19+95	-	20+20	LT	BRAUND	174
20+50	-	20+75	LT	BRAUND	214
20+50	-	20+75	RT	BRAUND	112
PROJECT 5991-00-55 CATEGORY 0010					
101+60	-	106+40	LT	PH	2024
101+75		102+00	RT	PH	132
102+66	-	102+71	RT	PH	24
103+16	-	103+21	RT	PH	25
104+06	-	104+11	RT	PH	24
104+51	-	104+61	RT	PH	48
104+76	-	104+91	RT	PH	73
106+67	-	106+72	RT	PH	24
106+85	-	106+95	LT	PH	43
108+96	-	109+01	LT	PH	25
109+19	-	109+24	LT	PH	25
109+58	-	109+84	LT	PH	127
111+70	-	111+80	LT	PH	49
				PROJECT TOTALS	5433

CURB RAMP DETECTABLE WARNING FIELD NATURAL PATINA

STATION	DIR	LOCATION	602.0515 SF
PROJECT 5991-00-54 CATEGORY 0010			
12+75	LT	BRAUND	22
13+20	LT	BRAUND	10
20+15	LT	BRAUND	20
20+15	RT	BRAUND	20
20+75	LT	BRAUND	20
20+75	RT	BRAUND	20
PROJECT 5991-00-55 CATEGORY 0010			
106+30	LT	PH	10
106+90	LT	PH	10
		PROJECT TOTALS	132

STORM SEWER PIPE

FROM	TO	LOCATION	608.0312 REIN. CONC. CLASS III 12-INCH LF
PROJECT 5991-00-54 CATEGORY 0010			
INL-XX	MH-EX	BRAUND	19
		PROJECT TOTALS	19

STORM SEWER STRUCTURES

STRUCTURE NUMBER	STA	DIR	LOCATION	611.0639 INLET COVERS TYPE H-S EACH	611.3230 INLETS 2X3-FT EACH	SPV.0060.02 INLET COVERS TYPE DW EACH
PROJECT 5991-00-54 CATEGORY 0010						
INL-XX	20+22	RT	BRAUND	1	1	
PROJECT 5991-00-55 CATEGORY 0010						
EXISTING	101+00	LT	PH			1
			PROJECT TOTAL	1	1	1

ADJUSTING UTILITIES

STATION	DIR	LOCATION	611.8110 ADJUSTING MANHOLE COVERS EACH	SPV.0060.01 EXISTING VALVE BOX ADJUSTMENT EACH
PROJECT 5991-00-54 CATEGORY 0010				
17+20	RT	BRAUND	1	
20+20	LT	BRAUND	1	
20+25	RT	BRAUND	1	
PROJECT 5991-00-54 CATEGORY 0020				
13+20	LT	BRAUND	1	
13+75	RT	BRAUND		1
16+60	L/R	BRAUND	2	2
19+95	RT	BRAUND		1
20+05	LT	BRAUND	1	
20+40	LT	BRAUND	1	
20+50	RT	BRAUND	1	1
PROJECT 5991-00-55 CATEGORY 0010				
106+55	LT	PH	1	
PROJECT 5991-00-55 CATEGORY 0020				
102+25	LT	PH		1
105+90	CL	PH	1	
106+05	LT	PH		1
106+55	LT	PH	1	
106+70	LT	PH	1	
107+20	L/R	PH	1	1
107+25	CL	PH	1	
110+60	CL	PH	1	
111+15	LT	PH		1
111+55	LT	PH		1
PROJECT TOTALS			16	10

LANDSCAPING ITEMS

STATION	TO	STATION	DIR	LOCATION	625.0100 TOPSOIL SY	627.0200 MULCHING SY	628.1905 MOBILIZATION EROSION CONTROL EACH	628.1910 MOBILIZATION EMERGENCY EROSION CONTROL EACH	629.0210 FERTILIZER TYP B CWT	630.0140 SEEDING MIXTURE NO. 40 LB
PROJECT 5991-00-54 CATEGORY 0010										
11+46.22	-	12+75	LT	BRAUND	47	47			0.03	0.84
13+20	-	13+35	LT	BRAUND	11	11			0.01	0.19
16+21	-	16+31	RT	BRAUND	2	2			0.01	0.04
16+52	-	16+70	RT	BRAUND	4	4			0.01	0.07
16+98	-	17+08	RT	BRAUND	2	2			0.01	0.04
19+03	-	19+13	RT	BRAUND	2	2			0.01	0.04
19+44	-	20+20	RT	BRAUND	17	17			0.02	0.31
19+95	-	20+20	LT	BRAUND	5	5			0.01	0.08
20+50	-	20+75	LT	BRAUND	11	11			0.01	0.19
20+50	-	20+75	RT	BRAUND	2	2			0.01	0.04
UNDISTRIBUTED					26	26	2	2	0.04	0.46
PROJECT 5991-00-55 CATEGORY 0010										
101+60	-	106+40	LT	PH	103	103			0.07	1.85
101+75		102+00	RT	PH	7	7			0.01	0.12
102+66	-	102+71	RT	PH	1	1			0.01	0.02
103+16	-	103+21	RT	PH	1	1			0.01	0.02
104+06	-	104+11	RT	PH	1	1			0.01	0.02
104+51	-	104+61	RT	PH	2	2			0.01	0.04
104+76	-	104+91	RT	PH	3	3			0.01	0.06
106+67	-	106+72	RT	PH	1	1			0.01	0.02
106+85	-	106+95	LT	PH	1	1			0.01	0.03
106+97	-	107+27	RT	PH	7	7			0.01	0.12
108+96	-	109+01	LT	PH	1	1			0.01	0.02
109+19	-	109+24	LT	PH	1	1			0.01	0.02
109+58	-	109+84	LT	PH	6	6			0.01	0.10
111+70	-	111+80	LT	PH	2	2			0.01	0.04
UNDISTRIBUTED					35	35	2	2	0.05	0.62
PROJECT TOTAL					301	301	4	4	0.42	5.42

INLET PROTECTION

STATION	DIR	LOCATION	628.7015 INLET PROTECTION TYPE C EACH
PROJECT 5991-00-54 CATEGORY 0010			
16+30	RT	BRAUND	1
19+85	LT	BRAUND	1
20+22	RT	BRAUND	1
PROJECT 5991-00-55 CATEGORY 0010			
100+80	RT	PH	1
101+00	LT	PH	1
106+22	LT	PH	1
106+32	RT	PH	1
109+68	RT	PH	1
109+73	LT	PH	1
PROJECT TOTALS			9

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SIGNING ITEMS

ZONE	SIGN NUMBER	LOCATION	SIGN CODE	SIZE	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2215 SIGNS TYPE II REFLECTIVE H FOLDING SF
PROJECT 5991-00-54 CATEGORY 0010									
	1-00R	BRAUND	REMOVE		1	1			
	1-01	BRAUND	R6-2-L	24x30			1	5	
	1-02R	BRAUND	REMOVE		1	0			
	1-03	BRAUND	R4-7	24x30			--	5	
	1-04R	BRAUND	REMOVE		1	0			
	1-05	BRAUND	R1-1F	36x36			--		7.46
	1-06R	BRAUND	REMOVE		1	0			
	1-07	BRAUND	R6-3	30X24			--	5	
		BRAUND	R1-1F	36x36					7.46
1	1-08R	BRAUND	REMOVE		1	1			
	1-09	BRAUND	R4-7	24X30			1	5	
	1-10R	BRAUND	REMOVE		1	1			
	1-11	BRAUND	R3-8-A	36X30			1	7.5	
	1-12	BRAUND	R3-17	30X24			1	5	
	1-13	BRAUND	R7-17	30X24			1	5	
	1-14R	BRAUND	REMOVE		1	1			
	1-15	BRAUND	R1-1	30X30			1	5.18	
	1-16R	BRAUND	REMOVE		1	1			
	1-17	BRAUND	R3-8-A	36X30			1	7.5	
	1-18	BRAUND	R3-17-BP	30X12			1	2.5	
	1-19R	BRAUND	REMOVE		1	1			
	1-20	BRAUND	R2-1	24X30			1	5	
	1-21R	BRAUND	REMOVE		1	1			
	1-22	BRAUND	R7-53D	18X24			1	3	
	2-00R	BRAUND	REMOVE		1	1			
	2-01	BRAUND	R7-53D	18X24			1	3	
	2-02	BRAUND	R4-4	36X30			1	7.5	
	2-03	BRAUND	R7-51R	18X24			1	3	
	2-04R	BRAUND	REMOVE		1	1			
2	2-05	BRAUND	R3-8	36X30			1	7.5	
	2-06	BRAUND	R3-17	30X24			1	5	
	2-07R	BRAUND	REMOVE		1	1			
	2-08	BRAUND	R1-1	36x36			1	7.46	
	2-08	BRAUND	R1-3P	18X6				0.75	
	2-09R	BRAUND	REMOVE		1	1			
	2-10	BRAUND	R1-1	36x36			1	7.46	
	2-10	BRAUND	R1-3P	18X6				0.75	
	3-00R	PH	REMOVE		1	1			
	3-01	PH	R1-1	30X30			1	6.25	
	3-01	PH	R1-3P	18X6				0.75	
				PROJECT TOTALS	15	12	18	110.1	14.92

PROJECT 5991-00-55 CATEGORY 0010									
	3-02R	PH	REMOVE		1	1			
	3-03	PH	R1-1	36x36			1	7.46	
	3-04	PH	R1-3P	18X6				0.75	
	3-05R	PH	REMOVE		1	1			
	3-06	PH	R7-53D	18X24			1	3	
	3-07R	PH	REMOVE		1	1			
	3-08	PH	R2-1	24X30			1	5	
	3-09R	PH	REMOVE		1	1			
	3-10	PH	R3-8	36X30			1	7.5	
	3-10	PH		18X24			1	3	
	3-11R	PH	REMOVE		1	1			
	3-12	PH	R7-53D	18X24			1	3	
	4-00R	PH	REMOVE		1	1			
	4-01	PH	R2-1	24X30			1	5	
	4-02R	PH	REMOVE		1	1			
	4-03	PH	R1-1	30X30			1	5.18	
	4-03	PH	R1-3P	18X6				0.75	
	4-04R	PH	REMOVE		1	1			
3	4-05	PH	R7-53D	18X24			1	3	
	4-06R	PH	REMOVE		1	1			
	4-07	PH	R7-53D	18X24			1	3	
	4-08R	PH	REMOVE		1	1			
	4-09	PH	R7-53D	18X24			1	3	
	4-10R	PH	REMOVE		1	1			
	4-11	PH	R7-53D	18X24			1	3	
	4-12	PH	R3-17-BP	30X12			1	2.5	
	4-12	PH	R3-17	30X24			1	5	
	5-00R	PH	REMOVE		1	1			
	5-01	PH	R7-53D	18X24			1	3	
	5-02	PH	RE-17	30X24			1	5	
	5-03R	PH	REMOVE		1	1			
	5-04	PH	R7-53D	18X24			1	3	
	5-05	PH							
	5-06R	PH	REMOVE		1	1			
	5-07	PH	R1-1	30X30			1	5.18	
	5-07	PH	R1-3P	18X6			0	0.75	
	5-08R	THEATER ROAD	REMOVE		1	1			
	5-09	THEATER ROAD	R3-8K	36X30			1	7.5	
				PROJECT TOTALS	15	15	19	84.57	0

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TRAFFIC CONTROL

ZONE	LOCATION	APPROX. SERVICE PERIOD DAYS	643.0300 TRAFFIC CONTROL DRUMS NO.	643.0300 TRAFFIC CONTROL DRUMS DAYS	643.0410 TRAFFIC CONTROL BARRICADES TYPE II NO.	643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAYS	643.0420 TRAFFIC CONTROL BARRICADES TYPE III NO.	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAYS	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A NO.	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAYS	643.0900 TRAFFIC CONTROL SIGNS NO.	643.0900 TRAFFIC CONTROL SIGNS DAYS	643.5000 TRAFFIC CONTROL
PROJECT 5991-00-54 CATEGORY 0010													
	PROJECT												1
1	STH 16	2	10	20		0	15	30	30	60	6	12	
	STRIP MALL ENTRANCE DRIVE	2		0	1	2	7	14	18	36	4	8	
	STA 13+50	2		0	1	2	11	22	26	52	7	14	
2	STA 13+50	30		0	1	30	11	330	28	840	7	210	
	Braund/PH North Leg	30		0	1	30	7	210	20	600	7	210	
	Braund/PH West Leg	30		0	1	30	7	210	20	600	7	210	
	Braund/PH East Leg	30		0	1	30	7	210	20	600	7	210	
	5991-02-54 Subtotal			20	6	124	65	1026	162	2788	45	874	1
PROJECT 5991-00-55 CATEGORY 0010													
	PROJECT												1
3	Braund/PH East Leg	28		0	1	28	7	196	16	448	7	196	
	Lester Avenue	28		0	2	56	6	168	14	392	5	140	
	STA 112+00	28		0	1	28	7	196	16	448	7	196	
	Theater Road	28	21	588					21	588	4	112	
	5991-02-55 Subtotal			588	4	112	20	560	67	1876	23	644	1
	Project Totals			608		236		1586		4664		1518	2

PEDESTRIAN ACCOMMODATION

STATION	DIR	CROSSING LOCATION	644.1420.S TEMPORARY PEDESTRIAN SURFACE PLYWOOD SF	644.1601.S TEMPORARY CURB RAMP EACH
PROJECT 5991-00-54 CATEGORY 0010				
20+45	LT	CROSSING PH WEST OF BRAUND	152	2
20+45		CROSSING BRAUND NORTH OF PH	152	2
20+75	RT	CROSSING BRAUND NORTH OF PH	152	2
PROJECT 5991-00-55 CATEGORY 0010				
106+50	LT	CROSSING LESTER AVE	152	2
		PROJECT TOTAL	608	8

PAVEMENT MARKING

STATION	TO	STATION	ROADWAY	646.1020 MARKING LINE EPOXY 4-INCH (YELLOW) LF	646.1020 MARKING LINE EPOXY 4-INCH (WHITE) LF	646.1020 MARKING LINE EPOXY 4-INCH (YELLOW, 12.5' SKIP) LF	646.3020 MARKING LINE EPOXY 8-INCH (WHITE) LF	646.3020 MARKING LINE EPOXY 8-INCH (WHITE, 3' SKIP) LF	646.5020 MARKING ARROW EPOXY EACH	646.5220 MARKING SYMBOL EPOXY EACH	646.5120 MARKING WORD EPOXY EACH	646.6120 MARKING STOP LINE EPOXY 18-INCH LF	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	646.9300 MARKING REMOVAL SPECIAL MARKING EACH
PROJECT 5991-00-54 CATEGORY 0010														
11+38	-	12+59	BRAUND				129							
11+46.22	-	13+50	BRAUND	387	95									
13+50	-	20+86.19	BRAUND	434	1696	112.5								
11+46.22	-	20+86.19	BRAUND											
		11+40	BRAUND									26		
11+50	-	11+60	BRAUND						2					
12+75	-	13+25	BRAUND										92	
12+80	-	12+90	BRAUND								1			
12+15	-	12+25	BRAUND						2					
12+35	-	12+45	BRAUND								1			
12+50	-	12+65	BRAUND						1	1				
17+65	-	17+30	BRAUND						1	1				
17+83	-	19+00	BRAUND					30						
18+25	-	18+40	BRAUND						1	1				
19+00	-	19+10	BRAUND						2					
19+00	-	20+05	BRAUND				105							
19+10	-	19+30	BRAUND						1	1				
19+65	-	19+75	BRAUND						3	1				
		20+00	BRAUND									31		
		20+10	BRAUND										100	
20+15	-	20+50	BRAUND										148.5	
		21+75	BRAUND									22	83	
			PROJECT TOTALS	821	1791	112.5	234	30	13	5	2	79	423.5	0
PROJECT 5991-00-55 CATEGORY 0010														
		100+90	PH									18		
101+78	-	112+00	PH	204	2011	237.5								
		101+75	PH									20		
102+05	-	102+20	PH						2	2				
105+95	-	106+10	PH						1	1				
106+35	-	106+85	PH										82	
107+15	-	107+30	PH						1	1				
111+00	-	111+15	PH							1				
112+50	-	112+70	PH						1	1				
113+13	-	113+30	PH						2					3
			PROJECT TOTALS	204	2011	237.5	0	0	7	6	0	38	82	3

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CONSTRUCTION STAKING

STATION	TO	STATION	LOCATION	650.4000 CONSTRUCTION STAKING STORM SEWER EACH	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.5500 CONSTRUCTION STAKING CURB & GUTTER LF	650.9000 CONSTRUCTION STAKING CURB RAMPS EA	650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 01. 5991-02-54 LS	650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 02. 5991-02-55 LS	SPV.0900.02 CONSTRUCTION STAKING MULTI-USE PATH LF
PROJECT 5991-00-54 CATEGORY 0010											
11+46.22	-	12+75	BRAUND								348
11+46.22	-	20+86.19	BRAUND		940	940			1		
		12+00	BRAUND				56				
		12+75	BRAUND					1			
		13+00	BRAUND				75				
		13+20	BRAUND					1			
		20+00	BRAUND				22				
		20+00	BRAUND				51				
		20+15	BRAUND					4			
20+20	-	20+25	BRAUND	1							
		20+50	BRAUND				36				
		20+50	BRAUND				22				
		20+75	BRAUND					4			
PROJECT 5991-00-55 CATEGORY 0010											
100+61.34	-	112+00	PH		1139	1139				1	
		106+30	PH					1			
		106+50	PH				19				
		106+90	PH					1			
			PROJECT TOTALS	1	2079	2079	281	8	1	1	348

LOOP DETECTOR CONDUIT, CABLE, AND WIRE

STATION	DIR	LOCATION	652.0800 CONDUIT LOOP DETECTOR LF	655.0800 LOOP DETECTOR WIRE LF
PROJECT 5991-00-54 CATEGORY 0010				
12+55	LT	BRAUND	55	65
		PROJECT TOTALS	55	65

SAWCUTS

STATION	TO	STATION	DIR	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF
PROJECT 5991-00-54 CATEGORY 0010						
11+46.22	-		LT	BRAUND	27	5
11+46	-		RT	BRAUND	117	
11+46	-	12+25		BRAUND	173	
12+75	-	13+25	LT	ENTRANCE DRIVE	40	
13+20	-	13+35	LT	BRAUND		10
13+25	-	13+40	RT	BRAUND		5
16+21	-	16+31	RT	BRAUND		10
16+52	-	16+70	RT	BRAUND		10
16+98	-	17+08	RT	BRAUND		10
17+04	-	17+14	RT	BRAUND		5
17+77	-	17+87	RT	BRAUND		5
19+03	-	19+13	RT	BRAUND		10
19+05	-	19+84	RT	BRAUND		84
19+44	-	20+20	RT	BRAUND		10
19+95	-	20+20	LT	BRAUND		10
20+25	-	20+55	LT	PH	25	
20+50	-	20+75	LT	BRAUND		10
20+50	-	20+75	RT	BRAUND		10
20+86.19	-			BRAUND	40	
PROJECT 5991-00-55 CATEGORY 0010						
101+97	-	102+50	RT	PH		58
102+66	-	102+71	RT	PH		10
103+16	-	103+21	RT	PH		10
104+06	-	104+11	RT	PH		10
104+51	-	104+61	RT	PH		10
104+73	-	104+78	LT	PH		5
104+76	-	104+91	RT	PH		10
106+13	-	106+19	LT	PH		5
106+24	-	106+39	RT	PH		5
106+35	-	106+85	LT	PH/LESTER	40	
106+67	-	106+72	RT	PH		10
106+84	-	106+95	LT	PH		5
106+97	-	107+42	RT	PH		15
107+39	-	107+88	LT	PH		15
107+64	-	107+74	RT	PH		5
107+97	-	108+07	RT	PH		5
108+96	-	109+01	LT	PH		10
109+19	-	109+24	LT	PH		10
109+58	-	109+84	LT	PH		10
110+29	-	110+39	RT	PH		5
110+40	-	110+48	LT	PH		5
111+73	-	111+81	LT	PH		5
112+00				PH	40	
112+06	-	112+14	RT	PH		5
				PROJECT TOTALS	502	417

WALL MODULAR BLOCK GRAVITY LANDSCAPE

STATION	TO	STATION	DIR	LOCATION	SPV.0165.01 WALL MODULAR BLOCK GRAVITY LANDSCAPE SF
PROJECT 5991-00-54 CATEGORY 0010					
13+24	-	13+36	LT	BRAUND	44
PROJECT 5991-00-55 CATEGORY 0010					
102+05	-	103+20	LT	PH	476
				PROJECT TOTALS	520

CONVENTIONAL SYMBOLS

SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W OR HE LINE
PROPERTY LINE — P.L. —
LOT, TIE & OTHER MINOR LINES
SLOPE INTERCEPT
CORPORATE LIMITS
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC) — W (TYPE) —
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)
TEMPORARY LIMITED EASEMENT AREA
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)
TRANSMISSION STRUCTURES
BUILDING — TO BE REMOVED
BRIDGE

SECTION CORNER SYMBOL (18 / 23 24 / 16 15 / 9)
SECTION CORNER MONUMENT
SIGN — SIGN
GEODETIC SURVEY MONUMENT
SIXTEENTH CORNER MONUMENT
OFF-PREMISE SIGN — 1-25 — SIGN

R/W MONUMENT (TO BE SET)
NON-MONUMENTED R/W POINT
FOUND IRON PIN (1-INCH UNLESS NOTED) IP

	COMPENSABLE	NON-COMPENSABLE
ELECTRIC POLE	■	□
TELEPHONE POLE	●	○
PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)	◆	◇

ACCESS RESTRICTED BY ACQUISITION
NO ACCESS (BY STATUTORY AUTHORITY)
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)
NO ACCESS (NEW HIGHWAY)

PARCEL NUMBER 25
UTILITY NUMBER 40
PARALLEL OFFSETS

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTICTIVE DEVELOPMENT EASEMENT	RDE
CENTERLINE	C/L	RIGHT	RT
CERTIFIED SURVEY MAP	CSM	RIGHT OF WAY	R/W
CONCRETE	CONC	SECTION	SEC
COUNTY	CO	SEPTIC VENT	SEPV
COUNTY TRUNK HIGHWAY	CTH	SQUARE FEET	SF
DISTANCE	DIST	STATE TRUNK HIGHWAY	STH
CORNER	COR	STATION	STA
DOCUMENT NUMBER	DOC	TELEPHONE PEDESTAL	TP
EASEMENT	EASE	TEMPORARY LIMITED EASEMENT	TLE
EXISTING	EX	TRANSPORTATION PROJECT PLAT	TPP
GAS VALVE	GV	UNITED STATES HIGHWAY	USH
GRID NORTH	GN	VOLUME	V
HIGHWAY EASEMENT	HE		
IDENTIFICATION	ID		
LAND CONTRACT	LC		
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), LACROSSE COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT OF WAY MONUMENTS ARE 3/4" X 24" AND ARE PLACED PRIOR TO OR AT THE TIME OF LAND TITLE TRANSFER.

RIGHT OF WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD.

CONVENTIONAL UTILITY SYMBOLS

WATER	W
GAS	G
TELEPHONE	T
OVERHEAD TRANSMISSION LINES	OH
ELECTRIC	E
CABLE TELEVISION	TV
FIBER OPTIC	FO
SANITARY SEWER	SAN
STORM SEWER	SS

CAUTION
THIS PLAT IS FOR ILLUSTRATIVE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES.

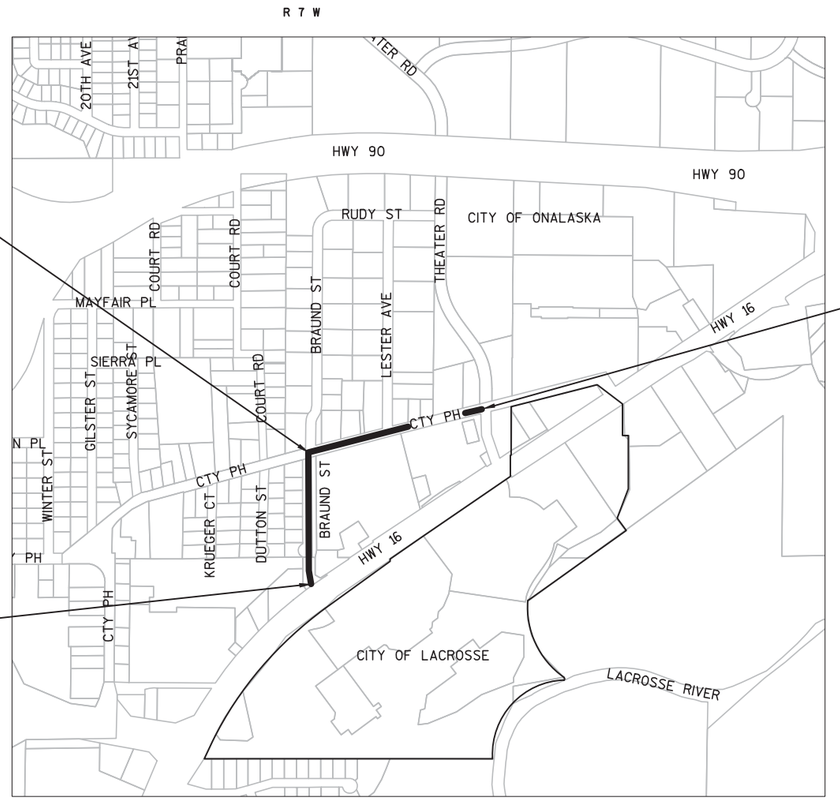
Right of Way information should be viewed in DOTView using the Real Estate Project ID. The Plat information contained in this AsBuilt Plan may not be the final records.

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
5991-02-53		
R/W PROJECT NUMBER --	4.01	5

PLAT OF RIGHT OF WAY REQUIRED FOR

CITY OF ONALASKA, BRAUND STREET & PH (STH 16 TO THEATER ROAD)

LOCAL STREET — LA CROSSE COUNTY



STA 20+43.37 BRAUND STREET =
STA 101+31.85 CTH PH
Y= 153313.935
X= 458826.835

BEGIN RELOCATION ORDER
5991-02-53
STA 11+32.77
Y= 152409.787
X= 458854.382
198.284 FEET SOUTH AND 0.582 FEET WEST OF SOUTH 1/4 SECTION 10, T16N, R7W

END RELOCATION ORDER
5991-02-53
STA 112+00.00
Y=153592.841
X=459857.926
972.963 FEET NORTH AND 1009.404 FEET EAST OF THE SOUTH 1/4 SECTION 10, T16N, R7W

LAYOUT
SCALE 0 — 1000 FT

TOTAL NET LENGTH OF CENTERLINE = 0.375 MI

ORIGINAL PLANS PREPARED BY

SEH

WISCONSIN
JASON L. CANCE
S-2688
CHIPPEWA FALLS WI
LAND SURVEYOR

2-7-2017 (Date)
(Signature)

REVISION DATE

CITY OF ONALASKA

APPROVED FOR THE CITY

DATE: _____ (Signatu

FILE NAME : P:\KO\O\ONALA\133149\C3D\RW\TITLE SHEET BRAUND ST.DWG
LAYOUT NAME - TITLE SHEET BRAUND ST - TITLE SHEET

PLOT DATE : 2/7/2017 9:44 AM PLOT BY : NICK ENGH PLOT NAME :

WISDOT/CADDS SHEET 50

SCHEDULE OF LANDS & INTERESTS REQUIRED				OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY OF ONALASKA.			
PARCEL NUMBER	SHEET NUMBER	OWNER (S)	INTEREST REQUIRED	R/W SF REQUIRED			TLE SF
				NEW	EXISTING	TOTAL	
1	4.04	VALLEY PLAZA LLC	FEE/TLE	792	-	792	552
2	4.04	JLP ASSOCIATES II OF EDEN PRAIRIE	FEE/TLE	1417	-	1417	271
3	4.04	DEAN A BLUSKE, DIANE E. OLSON, AUDREY L. FREDERICK, AND JANELLE A KOWALSKI	FEE	75	-	75	-
4	4.04	THE LKE LIMITED PARTNERSHIP	TLE	-	-	-	105
5	4.04	BENJAMIN R. MILLER & CRYSTAL D. MILLER	FEE/TLE	223	-	223	206
6	4.04/4.05	SPIRIT SPE PORTFOLIO 2006-1 LLC	FEE/TLE	181	-	181	1573
7	4.04	DENNIS T TRUE & VICKI R TRUE	FEE/TLE	688	-	688	783
10	4.04/4.05	JAE ENTERPRISES	TLE	-	-	-	934
11	4.05	CENTURYTEL OF WISCONSIN	TLE	-	-	-	711
12	4.05	VALLEY VIEW BUSINESS PARK	TLE	-	-	-	50
13	4.05	THE AUDIO VIDEO PROS INC	TLE	-	-	-	359
14	4.05	CMJ INVESTMENTS LLC	TLE	-	-	-	211
100	4.04	WISCONSIN-MINNESOTA LIGHT AND POWER COMPANY	RELEASE OF RIGHTS				
101	4.05	NORTHERN STATES POWER COMPANY	RELEASE OF RIGHTS				

R/W COORDINATES		
Point	North	East
550	152607.475	458768.514
551	152607.527	458776.594
556	152398.668	458804.626
557	152422.427	458799.079
558	152439.135	458791.618
559	152487.105	458784.245
560	152535.773	458781.554
561	152535.485	458769.557
562	152626.992	458777.598
584	153269.393	458864.691
585	153284.761	458872.172
586	153297.321	458891.565
598	153362.857	458859.475
599	153402.015	459003.098
600	153395.914	459004.748
638	152432.189	458791.738
641	152625.116	458789.050

REVISION DATE	DATE 2-7-2017	NOT TO SCALE	HWY: BRAUND RD-CTH PH	STATE R/W PROJECT NUMBER 5991-02-53	PLAT SHEET 4.02	
			COUNTY: LACROSSE	CONSTRUCTION PROJECT NUMBER 5991-02-54/55	PS&E SHEET --24	E

FILE NAME : SCHEDULE OF LANDS.DWG
LAYOUT NAME - SCHEDULE OF LANDS - -4.02

PLOT DATE : 2/7/2017 9:44 AM PLOT BY : NICK ENGH PLOT NAME : PLOT SCALE : **********

WISDOT/CADDS SHEET 60

CITY

OF

STA 20+43.37 BRAUND STREET =
STA 101+31.85 CTH PH
Y= 153313.935
X= 458826.835

BRAUND ST

LESTER AVE

THEATER RD

105

CTH PH

110

20

ONALASKA

THEATER RD

END RELOCATION ORDER
STA 112+00.00
Y=153592.841
X=459857.926

GN

4

4

SE/SW

DUTTON ST

BRAUND ST

15

STH 16

SW/SE

CITY

BEGIN RELOCATION ORDER
STA 11+32.77
Y= 152409.787
X= 458854.382

OF

NE/NW

NW/NE

LACROSSE

REVISION DATE	DATE 2-7-2017	NOT TO SCALE	HWY: BRAUND ST/CTH PH	STATE R/W PROJECT NUMBER 5991-02-53	PLAT SHEET 4.03	
	GRID FACTOR N/A		COUNTY: LACROSSE	CONSTRUCTION PROJECT NUMBER 5991-02-54/55	PS&E SHEET 25	E

FILE NAME : P:\KO\O\Onala\133149\C3d\RW\PROJCT OVERVIEW.dwg
LAYOUT NAME - PROJCT OVERVIEW - 4.12

PLOT DATE : 2/7/2017 9:45 AM PLOT BY : NICK ENGH PLOT NAME : PLOT SCALE : Custom

WISDOT/CADDS SHEET 75

Point	Station	Offset
550	13+28.00	59.00' LT
551	13+29.00	51.00' LT
552	12+75.00	54.03' LT
553	12+75.00	59.00' LT
554	13+25.00	63.00' LT
555	13+25.00	58.50' LT
556	11+45.44	49.00' LT
557	11+65.00	44.28' LT
558	11+80.00	45.73' LT
559	12+20.00	43.00' LT
560	12+65.00	42.00' LT
561	12+65.00	54.00' LT
562	13+47.03	53.00' LT
563	20+64.74	47.36' LT
564	20+83.00	32.38' LT
571	12+55.00	40.00' LT
572	17+13.00	36.92' RT
573	17+13.00	40.00' RT
574	16+16.00	40.00' RT
575	16+16.00	36.94' RT
576	18+97.00	36.88' RT
577	18+97.00	40.00' RT
578	19+18.00	40.00' RT
579	19+18.00	36.88' RT
580	19+39.00	36.87' RT
581	19+39.00	40.00' RT
582	19+94.00	40.00' RT
583	20+27.70	74.06' RT
584	19+98.00	36.86' RT
585	20+13.20	44.68' RT
586	20+25.33	64.35' RT
587	102+60.00	33.14' RT
588	102+60.00	38.00' RT
589	102+75.00	38.00' RT
590	102+75.15	33.18' RT
591	103+10.00	33.28' RT
592	103+10.00	38.00' RT
593	103+25.00	38.00' RT
594	103+25.00	33.32' RT
595	101+77.50	44.00' LT
596	103+50.00	44.00' LT
597	103+50.00	38.00' LT
598	101+76.21	39.00' LT
599	103+25.00	39.00' LT
600	103+25.00	32.68' LT
628	11+32.77	0.00' LT
629	13+47.03	0.00' LT
641	13+47.03	41.39' LT
638	11+74.58	47.79' LT
642	11+75.00	53.00' LT
643	12+20.00	48.00' LT
644	12+65.00	47.00' LT

HISTORICAL BASIS OF R/W	
CSM VOL 2, PG 211 DOC# 955842	8/24/1984
CSM VOL 7, PG 115 DOC# 752031	1/2/1997
OAKLAND ADDITION DOC# 698251	4/27/1959
VALLEY VIEW BUSINESS PARK ADDITION DOC# 968077	4/30/1985
RELOCATION ORDER CO-181	6/11/1985

CITY

ONALASKA

NE/NW 15-16-7

NW/NE 15-16-7

SW/SE 10-16-7

SE/SW 10-16-7

DUTTON ST

60'

RECIPROCAL EASEMENT
DOC # 943476
DOC # 948182
DOC # 971028
DOC # 1022733
DOC # 1035421

RECIPROCAL EASEMENT
DOC # 943476
DOC # 948182
DOC # 971028
DOC # 1035421

(2) JLP ASSOCIATES II OF EDEN PRAIRIE

INGRESS/EGRESS EASEMENT
DOC # 1022736

RESTRICTED ACCESS
DOC # 997082

SLOPE INTERCEPT

(1) VALLEY PLAZA LLC
TLE SLOPE
552 S.F.

PI: 11+77.65

PI: 12+86.03

TLE SLOPE
271 S.F.

24'

(100) BLANKET EASEMENT
WISCONSIN-MINNESOTA LIGHT AND POWER COMPANY
DOC # 246813

(3) DEAN A BLUSKE, DIANE E. OLSON, AUDREY L. FREDERICK, AND JANELLE A KOWALSKI

LOT 13

LOT 6 LOT 5 LOT 4 LOT 3 LOT 2 LOT 1

BLOCK TWO OAKLAND ADDITION DOC# 698251

LOT 11 LOT 10 LOT 9 LOT 8 LOT 7

90'

N74° 51' 50"E

STA 101+43.54 CTH PH

66'

COUNTY ROAD PH

N74° 51' 50"E 101+00

STA 20+43.37 BRAUND STREET =
STA 101+31.85 CTH PH
Y= 153313.935
X= 458826.835

BLOCK ONE VALLEY VIEW BUSINESS PARK ADDITION DOC# 968077

BP: 100+00.00

LOT 1 LOT 2 LOT 3 LOT 4

TLE SLOPE
105 S.F.

563
Y= 153308.662
X= 458807.340

EP: 21+52.31

PI: 101+11.66

(4) THE LKE LIMITED PARTNERSHIP

564
Y= 153319.5032
X= 458847.420

PI: 101+53.18

TLE SLOPE
206 S.F.

(5) BENJAMIN R. MILLER & CRYSTAL D. MILLER

OUTLOT 1

3/4" REBAR
Y= 155257.980
X= 458809.130

BLOCK TWO VALLEY VIEW BUSINESS PARK ADDITION DOC# 968077

(7) DENNIS T TRUE & VICKI R TRUE

TLE SLOPE
783 S.F.

SLOPE INTERCEPT

OUTLOT 2

TLE SLOPE
188 S.F.

(10) JAE ENTERPRISES LLC

MATCHLINE STA 103+75

3/4" REBAR

67'

69'

66'

102+00

103+00

N74° 51' 50"E

TLE SLOPE
65 S.F.

TLE SLOPE
501 S.F.

TLE SLOPE
74 S.F.

TLE SLOPE
71 S.F.

(6) SPIRIT SPE PORTFOLIO 2006-1 LLC

CROSS EASEMENT AGREEMENT
BLANKET DESCRIPTION
DOC # 955171
DOC # 990021
DOC # 1016928

LOT 1 CSM VOL 7 PG 115 DOC# 752031

LOT 1 CSM VOL 2 PG 211 DOC# 955842

LOT 2 CSM VOL 7 PG 115 DOC# 752031

BEGIN RELOCATION ORDER
STA 11+32.77
Y= 152409.787
X= 458854.382

N30° 27' 59"W

BP: 10+00.00

11+00

12+00

13+00

14+00

15+00

16+00 BRAUND ST

17+00

18+00 N1° 16' 01"W

19+00

20+00

STH 16

PC: 11+32.77

PT: 12+21.17

PC: 12+31.88

80'

PT: 13+39.53

PC: 13+77.16

PI: 14+14.16

PT: 14+50.95

1" IP
Y= 152619.878
X= 458848.522

PI STA = 11+77.65
PIY = 152448.467
PIX = 458831.628
DELTA = 24°20'57"
D = 27°32'46"
T = 44.87'
L = 88.39'

PI STA = 12+86.03
PIY = 152557.580
PIX = 458819.934
DELTA = 15°25'12"
D = 14°19'26"
T = 54.15'
L = 107.65'

PI STA = 14+14.16
PIY = 152684.672
PIX = 458840.753
DELTA = 10°34'12"
D = 14°19'26"
T = 37.00'
L = 73.79'

APPROXIMATE SECTION LINE

From	To	Direction	Distance
1101	1100	S00°51'20"E	2638.39'
1100	629	S85°32'08"W	18.68'
629	630	S09°18'10"W	7.50'
631	632	S06°07'02"E	10.71'
628	556	S77°24'06"W	50.98'
556	557	N13°08'24"W	24.40'
557	638	N36°56'42"W	12.21'
638	558	N00°59'23"W	6.95'
558	559	N08°44'16"W	48.53'
559	560	N03°09'52"W	48.74'
560	561	S88°37'39"W	12.00'
561	550	N00°49'49"W	72.00'
550	551	N89°37'50"E	8.08'
551	562	N02°57'05"E	19.49'
562	629	S80°41'49"E	53.00'

From	To	Direction	Distance
1100	634	N00°51'20"W	737.07'
634	598	N74°44'57"E	22.76'
598	599	N74°44'57"E	148.86'
599	600	S15°08'10"E	6.32'
600	635	S15°08'10"E	32.68'
635	636	S74°51'50"W	135.00'
636	586	S15°08'10"E	32.94'
586	585	S57°04'11"W	23.10'
585	584	S25°57'22"W	17.09'
584	637	S88°43'59"W	25.87'
637	634	N00°51'20"W	88.06'

From	To	Direction	Distance
562	641	S80°41'49"E	11.61'
641	629	S80°41'49"E	41.39'

Element	Length	Radius	Direction	Distance
CURVE 630-631	107.65'	400.00 L	S01°35'34"W	107.33'
CURVE 632-628	88.39'	208.00 L	S18°17'30"E	87.73'

REVISION DATE	DATE 2-7-2017	SCALE. FEET 0 50 100	HWY:BRAUND ST / CTH PH	STATE R/W PROJECT NUMBER 5991-02-53	PLAT SHEET 4.04
	GRID FACTOR N/A		COUNTY: LACROSSE	CONSTRUCTION PROJECT NUMBER 5991-02-54/55	PS&E SHEET 26 E

FILE NAME : 4.08 BRAUND-PH.DWG
LAYOUT NAME - 4.08 BRAUND-PH - 4.08

PLOT DATE : 2/7/2017 9:46 AM PLOT BY : NICK ENGH PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 75

CITY

OF

ONALASKA

4

4

GN

LESTER AVE

THEATER RD

80'

66'

PL

LOT 2

LOT 1

LOT 13

BLOCK TWO VALLEY VIEW BUSINESS PARK ADDITION DOC# 968077

BLOCK THREE VALLEY VIEW BUSINESS PARK ADDITION DOC# 968077

(10) JAE ENTERPRISES LLC

(11) CENTURYTEL OF WISCONSIN LLC

(12) VALLEY VIEW BUSINESS PARK

(13) THE AUDIO VIDEO PROS INC

(14) CMJ INVESTMENTS LLC

LOT 2 CSM VOL 7 PG 77 DOC# 1161531

TLE SLOPE 88 S.F.

TLE SLOPE 733 S.F.

SLOPE INTERCEPT

TLE SLOPE 711 S.F.

TLE SLOPE 50 S.F.

TLE SLOPE 229 S.F.

TLE SLOPE 130 S.F.

TLE SLOPE 88 S.F.

TLE SLOPE 123 S.F.

569 570 601 602 603 604 605 606 607 608 609 610 611 612 613 614 615 616 617 618 619 620 621 622 623 624 625 626 627

IP

MATCHLINE STA 103+75

104+00 105+00 106+00 107+00 108+00 109+00 110+00 111+00 112+00 113+00 114+00

99'

N74° 51' 50"E

COUNTY ROAD PH

EP: 114+06.07

TLE SLOPE 213 S.F.

TLE SLOPE 67 S.F.

TLE SLOPE 285 S.F.

END RELOCATION ORDER
STA 112+00.00
Y= 153592.841
X= 459857.926

3/4" REBAR
Y= 155257.980
X= 458809.130

231.46'
N74° 51' 50"E
TO STATION
103+75

1" IP
Y= 152619.878
X= 458848.522

1101 T16N R7W S10

1100 T16N R7W S10 S15

CROSS EASEMENT AGREEMENT
BLANKET DESCRIPTION
DOC # 955171
DOC # 990021
DOC # 1016928

(101) NORTHERN STATES POWER COMPANY ELECTRIC & GAS DISTRIBUTION EASEMENT DOC # 1162031

(6) SPIRIT SPE PORTFOLIO 2006-1 LLC

LOT 1 CSM VOL 2 PG 211 DOC# 955842

LOT 2 CSM VOL 7 PG 115 DOC# 752031

LOT 2 CSM VOL 2 PG 211 DOC# 955842

CITY OF ONALASKA MUNICIPAL EASEMENT DOC # 962629

SW/SE
10-16-7

HISTORICAL BASIS OF R/W	
CSM VOL 2, PG 211 DOC# 955842	8/24/1984
CSM VOL 7, PG 115 DOC# 752031	1/2/1997
OAKLAND ADDITION DOC# 698251	4/27/1959
VALLEY VIEW BUSINESS PARK ADDITION DOC# 968077	4/30/1985
RELOCATION ORDER CO-181	6/11/1985

Point	Station	Offset
569	106+98.50	42.00' LT
570	107+06.00	32.07' LT
601	104+00.00	33.53' RT
602	104+00.00	38.00' RT
603	104+15.00	38.00' RT
604	104+15.00	33.57' RT
605	104+45.00	33.66' RT
606	104+45.00	38.00' RT
607	104+95.00	38.00' RT
608	104+95.00	33.80' RT.
609	104+99.00	32.19' LT
610	104+99.00	38.00' LT
611	106+29.29	38.00' LT
612	106+61.00	33.90' RT
613	106+61.00	38.00' RT
614	107+31.00	38.00' RT
615	107+31.00	33.94' RT
616	108+90.00	31.98' LT
617	108+90.00	38.00' LT
618	109+28.00	38.00' LT
619	109+28.00	31.97' LT
620	109+52.00	31.95' LT
621	109+52.00	38.00' LT
622	109+88.00	38.00' LT
623	109+88.00	31.93' LT
624	110+64.00	31.85' LT
625	110+64.00	38.00' LT
626	110+84.00	38.00' LT
627	110+84.00	31.83' LT

REVISION DATE	DATE 2-7-2017	SCALE. FEET 0 50 100	HWY: BRAUND ST / CTH PH	STATE R/W PROJECT NUMBER 5991-02-53	PLAT SHEET 4.05
	GRID FACTOR N/A		COUNTY: LACROSSE	CONSTRUCTION PROJECT NUMBER 5991-02-54/55	PS&E SHEET 27 E

FILE NAME : 4.08 BRAUND-PH.DWG
LAYOUT NAME - 4.08 BRAUND-PH - 4.09

PLOT DATE : 2/7/2017 9:46 AM PLOT BY : NICK ENGH PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 75

VALLEY PLAZA LLC

JLP ASSOCIATES II OF EDEN PRAIRIE

DEAN BLUSKE & JANELLE A KOUALSKI

AUDREY LITTLEWOLF

CAMILLA GRIMME

CHARTER

REPLACE LOOP DETECTOR
CUTOFF EXISTING CONDUIT AT CURB FLAG AND SPLICE NEW CONDUIT
REPLACE LOOP DETECTOR WIRE AND CABLE FROM PULL BOX

MULTI-USE PATH

ENTRANCE DRIVE

RETAINING WALL

INLET PROTECTION TYPE C IS UTILIZED THROUGHOUT THE PROJECT. SEE MISCELLANEOUS QUANTITY TABLE FOR LOCATIONS.

N

BORING B17
STA 13+44, 17' LT

BORING B19
STA 16+42, 16' LT

XCEL

CITY OF ONALASKA WATER

CITY OF ONALASKA SANITARY SEWER

PC: 11+32.77

PT: 12+21.17

PC: 12+31.88

PT: 13+39.53

PC: 13+77.16

PT: 14+50.95

12+00

N6° 07' 01"W
10.71'

13+00

B17

N9° 18' 10"E
37.63'

14+00

15+00

16+00

B19

B18

BRAUND STREET

MATCH LINE STA 16+50.00

N30° 27' 59"W
132.77'

11+00

Replaced Broken Grates
(Supplied by City)

By City

Done by Gerke

BEGIN PROJECT
5991-02-54
STA 11+46.22
MATCH EXISTING
SAWCUT REQ'D
Y=152421.594
X=458847.940

PI STA = 11+77.6
Y = 152448.467
X = 458831.628
DELTA = 24°20'57"
D = 27°32'46"
T = 44.87'
L = 88.39'
R = 208.00'
PC STA = 11+32.77
PT STA = 12+21.17

PI STA = 12+86.03
Y = 152557.580
X = 458819.934
DELTA = 15°25'12"
D = 14°19'26"
T = 54.15'
L = 107.65'
R = 400.00'
PC STA = 12+31.88
PT STA = 13+39.53

STA 13+25 TO STA 13+40
REMOVE CONCRETE C&G
CONCRETE C&G 30-INCH TYPE D

CENTURYLINK

PI STA = 14+14.16
Y = 152684.672
X = 458840.753
DELTA = 10°34'12"
D = 14°19'26"
T = 37.00'
L = 73.79'
R = 400.00'
PC STA = 13+77.16
PT STA = 14+50.95

BORING B18
STA 14+73, 14' RT

STA 16+21 TO STA 16+31
REMOVE CONCRETE SIDEWALK
CONCRETE SIDEWALK 4" (10'X5')

SPIRIT SPE PORTFOLIO

SPIRIT SPE PORTFOLIO

5

5

730
720
710
700

STA 12+00.00 EL 711.41
STA 12+50.00 EL 712.48
STA 13+00.00 EL 713.71
STA 13+50.00 EL 714.45
STA 14+00.00 EL 715.12
STA 14+50.00 EL 715.33
STA 15+00.00 EL 715.05
STA 15+17.64 EL 714.79
STA 15+50.00 EL 714.03
STA 16+00.00 EL 712.84
STA 16+50.00 EL 711.53

2.15% 2.46% 1.47% 1.34% 0.42% -0.56% -1.47% -2.36% -2.39% -2.61%

FINISHED ℄ PROFILE

EXISTING ℄ PROFILE

EXISTING ℄ ELEVATION

FINISHED ℄ ELEVATION

710.75 711.65 711.41 712.42 712.48 713.72 713.71 714.47 714.45 715.11 715.12 715.40 715.33 715.01 715.05 713.98 714.03 712.79 712.84 711.49 711.53

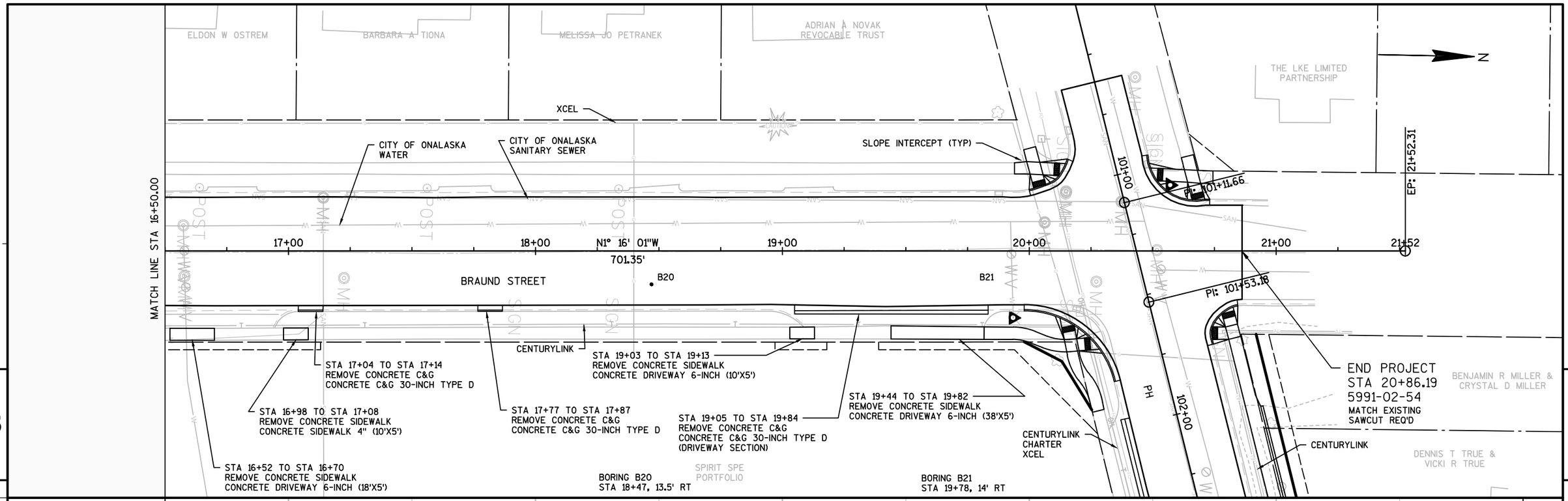
11+00 12+00 13+00 14+00 15+00 16+00 16+50

PROJECT NO: 5991-02-54	HWY: BRAUND STREET	COUNTY: LA CROSSE	PLAN AND PROFILE: BRAUND STREET	SHEET 28	E

FILE NAME : \\SEHLX1\PROJECTS\KO\O\ONALA\133149\C3D\SHEETSPLAN\BRAUND\050101-PP-BRAUND.DWG
LAYOUT NAME - 050101-PP-BRAUND - 050109

PLOT DATE : 7/19/2017 5:33 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

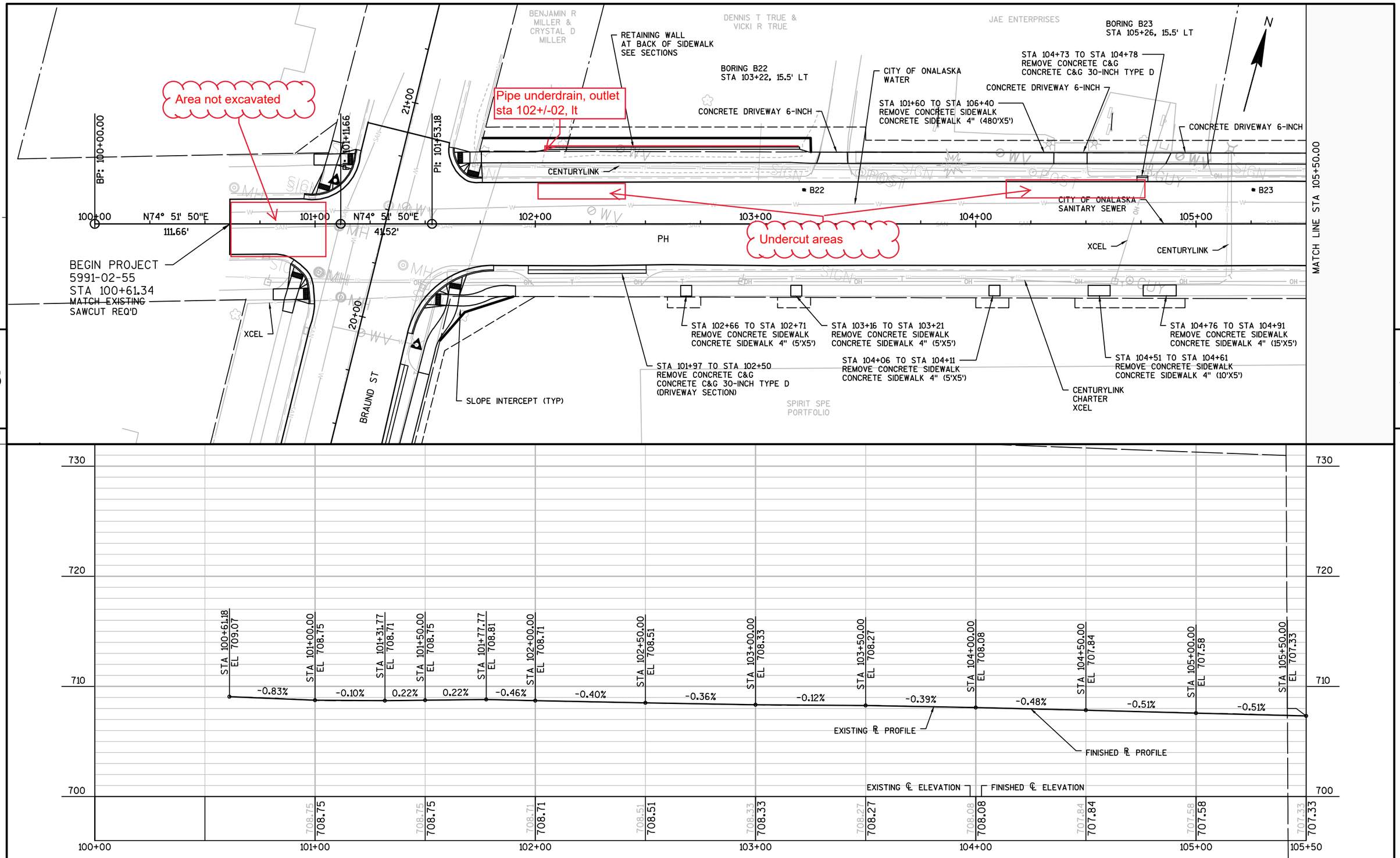


PROJECT NO: 5991-02-54	HWY: BRAUND STREET	COUNTY: LA CROSSE	PLAN AND PROFILE: BRAUND STREET	SHEET 29	E

FILE NAME : \\SEHLX1\PROJECTS\KO\O\ONALA\133149\C3D\SHEETSPLAN\BRAUND\050101-PP-BRAUND.DWG
LAYOUT NAME - 050101-PP-BRAUND - 050110

PLOT DATE : 6/8/2017 3:32 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44



PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	PLAN AND PROFILE: PH	SHEET 30	E

FILE NAME : \\SEHLX1\PROJECTS\KO\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\050101-PP-PH.DWG
LAYOUT NAME - 050101-PP-PH - 050109
PLOT DATE : 6/8/2017 3:37 PM
PLOT BY : TOREY LEONARD
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

Valve Box Removed
(Eliminated by City)

Replaced Broken Grate
(Supplied by City)

CENTURYTEL WISCONSIN

VALLEY VIEW BUSINESS PARK

AUDIO VIDEO PROS INC

CMJ INVESTMENTS INC

LESTER AV

LINE STA 105+50.00

MATCH LINE STA 111+00.00

STA 106+13 TO STA 106+19
REMOVE CONCRETE C&G
CONCRETE C&G 30-INCH TYPE D

CITY OF ONALASKA WATER

CENTURYLINK

BORING B24
STA 107+25, 15.5' LT

SLOPE INTERCEPT (TYP)

STA 107+39 TO STA 107+88
REMOVE CONCRETE C&G AND APRON
CONCRETE C&G 30-INCH TYPE D
CONCRETE DRIVEWAY 6-INCH

BORING B25
STA 109+27, 15.5' LT

STA 108+96 TO STA 109+01
REMOVE CONCRETE SIDEWALK
CONCRETE SIDEWALK 4" (5'X5')

STA 109+19 TO STA 109+24
REMOVE CONCRETE SIDEWALK
CONCRETE SIDEWALK 4" (5'X5')

STA 110+40 TO STA 110+48
REMOVE CONCRETE C&G
CONCRETE C&G 30-INCH TYPE D

STA 109+58 TO STA 109+84
REMOVE CONCRETE SIDEWALK
CONCRETE SIDEWALK 4" (26'X5')

STA 111+70 TO STA 111+80
REMOVE CONCRETE SIDEWALK
CONCRETE SIDEWALK 4" (10'X5')

XCEL

B24

B25

106+00 107+00 108+00 109+00 110+00

N74° 51' 50"E
1252.89'
PH

CHARTER
XCEL

STA 106+24 TO STA 106+39
REMOVE CONCRETE C&G
CONCRETE C&G 30-INCH TYPE D

STA 106+97 TO STA 107+27
REMOVE CONCRETE SIDEWALK
CONCRETE DRIVEWY 6-INCH (35'X5')

STA 106+67 TO STA 106+72
REMOVE CONCRETE SIDEWALK
CONCRETE SIDEWALK 4" (5'X5')

SPIRIT SPE PORTFOLIO

STA 106+84 TO STA 107+42
REMOVE CONCRETE C&G AND APRON
CONCRETE C&G 30-INCH TYPE D
CONCRETE DRIVEWAY 6-INCH

STA 107+64 TO STA 107+74
REMOVE CONCRETE C&G
CONCRETE C&G 30-INCH TYPE D

STA 107+97 TO STA 108+07
REMOVE CONCRETE C&G
CONCRETE C&G 30-INCH TYPE D

CENTURYLINK
DAIRYLAND
CHARTER
XCEL

CITY OF ONALASKA
SANITARY SEWER

STA 110+29 TO STA 110+39
REMOVE CONCRETE C&G
CONCRETE C&G 30-INCH TYPE D

GEORGE D PATHOS REVOCABLE TRUST

N

5

5

720
710
700
695

STA 105+50.00 EL 707.33
STA 106+00.00 EL 707.02
STA 106+50.00 EL 706.75
STA 106+67.65 EL 706.77
STA 107+00.00 EL 706.60
STA 107+27.29 EL 706.46
STA 107+50.00 EL 706.44
STA 108+00.00 EL 706.32
STA 108+50.00 EL 706.14
STA 109+00.00 EL 706.02
STA 109+50.00 EL 706.00
STA 109+81.91 EL 706.04
STA 110+00.00 EL 706.19
STA 110+50.00 EL 706.59
STA 111+00.00 EL 707.06

-0.61% -0.54% 0.12% -0.55% -0.49% -0.11% -0.22% -0.37% -0.24% -0.04% 0.12% 0.85% 0.79% 0.94%

EXISTING ℄ PROFILE

FINISHED ℄ PROFILE

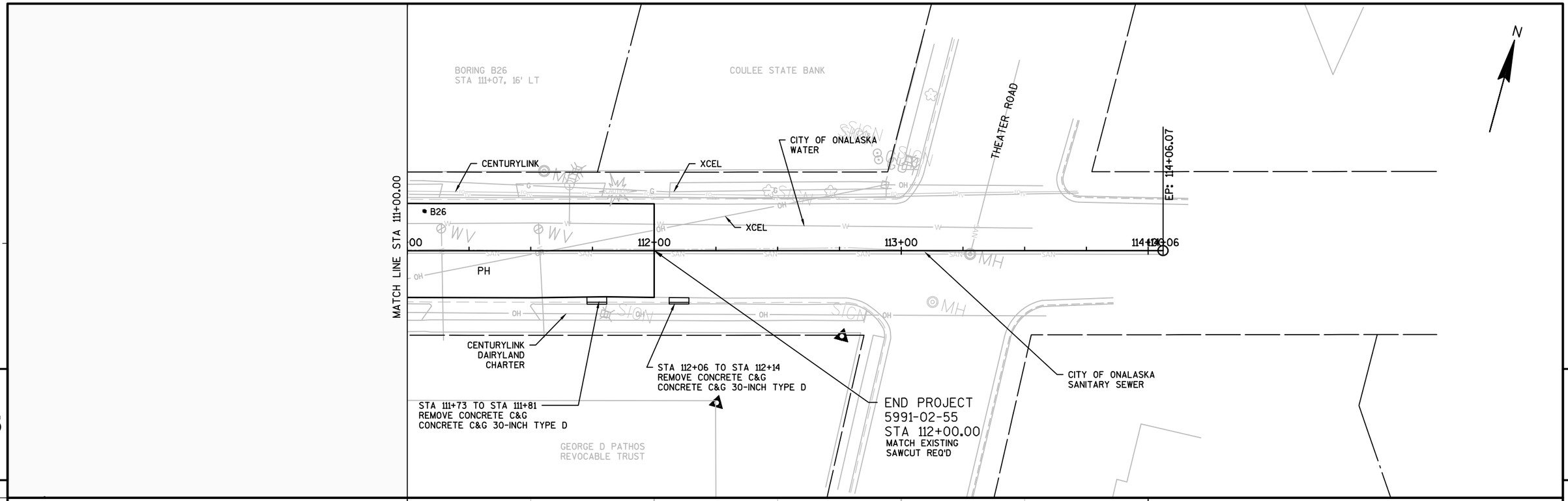
EXISTING ℄ ELEVATION

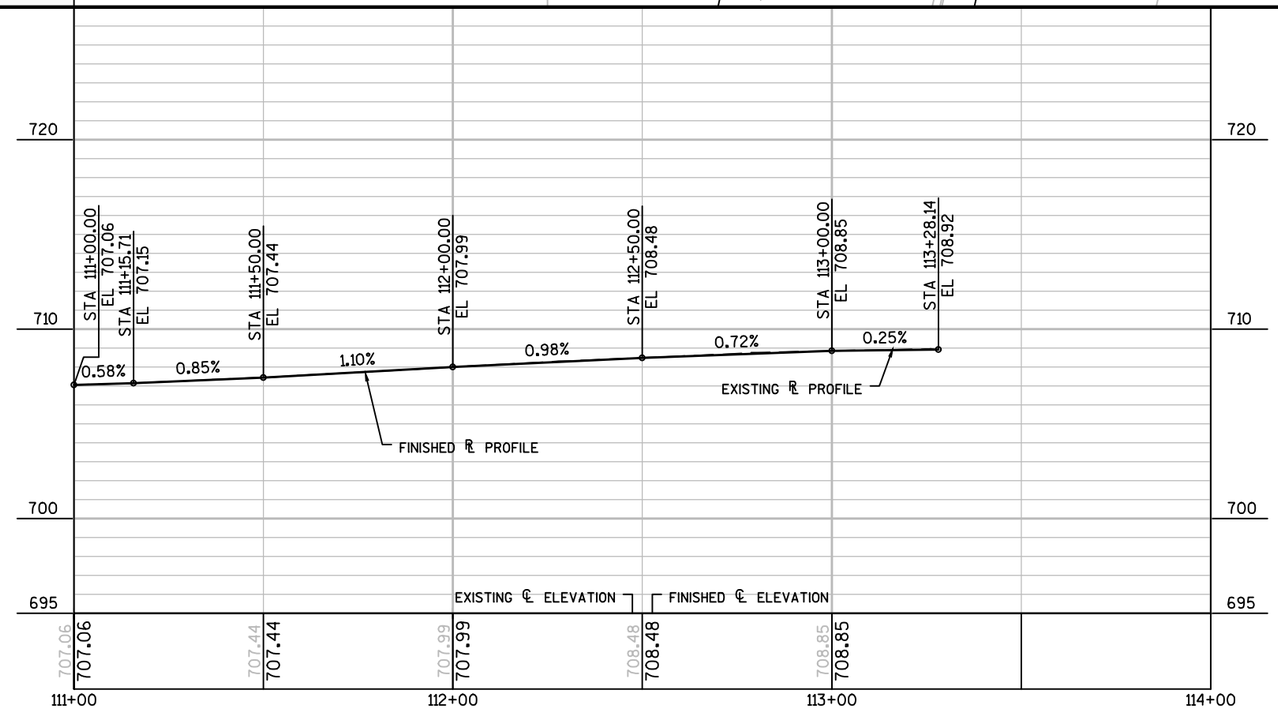
FINISHED ℄ ELEVATION

707.33 707.02 706.75 706.60 706.44 706.32 706.14 706.02 706.00 706.19 706.59 707.06

105+50 106+00 107+00 108+00 109+00 110+00 111+00

PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	PLAN AND PROFILE: PH	SHEET 31	E



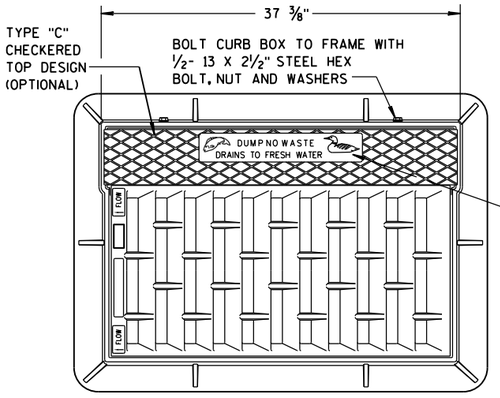


PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	PLAN AND PROFILE: PH	SHEET 32 E

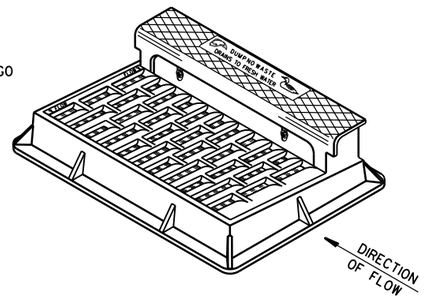
FILE NAME : \\SEHLX1\PROJECTS\K0\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\050101-PP-PH.DWG
LAYOUT NAME - 050101-PP-PH - 050111
PLOT DATE : 6/1/2017 10:31 AM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

Standard Detail Drawing List

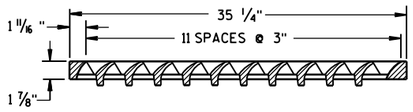
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08C07-02	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08D01-20A	CONCRETE CURB & GUTTER
08D01-20B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-19A	CURB RAMPS TYPES 1 AND 1-A
08D05-19B	CURB RAMPS TYPES 2 AND 3
08D05-19C	CURB RAMPS TYPES 4A AND 4A1
08D05-19D	CURB RAMPS TYPE 4B AND 4B1
08D05-19E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-19F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-19G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D18-01	DRIVEWAY AND SIDEWALK RAMPS TYPES X & Y
08D19-01	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09F08-04	LOOP DETECTOR PLACED IN CRUSHED AGGREGATE BASE (NEW ASPHALTIC PAVEMENT)
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C03-03	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C05-03	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C07-14A	PAVEMENT MARKING SYMBOLS
15C07-14B	PAVEMENT MARKING WORDS
15C07-14C	PAVEMENT MARKING ARROWS
15C07-14E	PAVEMENT MARKING FOR BIKE LANES
15C08-18A	LONGITUDINAL MARKING (MAINLINE)
15C08-18B	PAVEMENT MARKING (TURN LANES)
15C19-04A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C29-05A	BICYCLE LANE MARKING
15C29-05B	PAVEMENT MARKING FOR SHARED LANE 35 MPH OR LESS
15C33-02	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-01A	PAVEMENT MARKING (INTERSECTIONS)
15D30-03A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-03B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-03C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

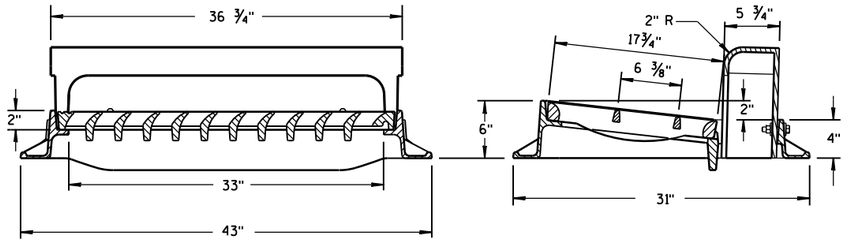


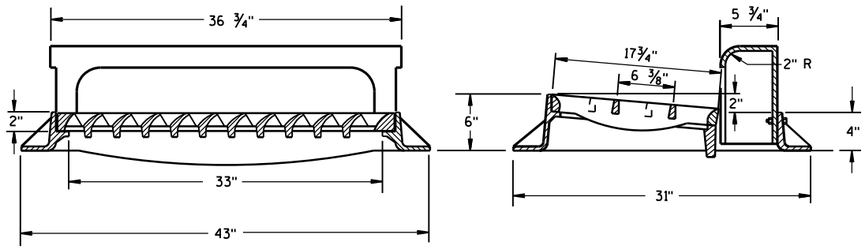
NOTE:
GRATE IS REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

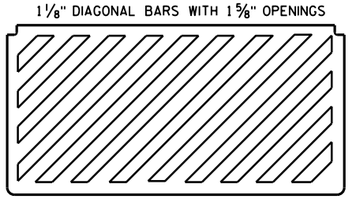






TYPE "H"

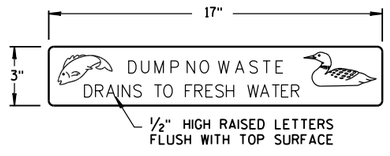
NOTE: EITHER CASTING IS ACCEPTABLE



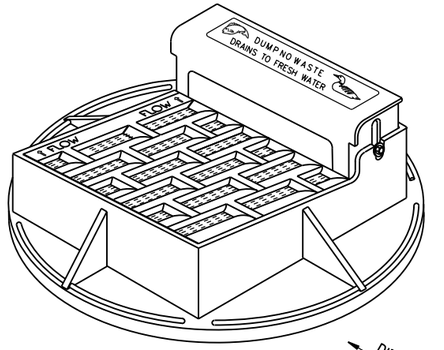
SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35 1/4" X 17 3/4" X 2")

(NOTED AS TYPE H-S ON DRAINAGE TABLE)



LOGO DETAIL



NOTE: CURB BOX ADJUSTABLE 4" TO 9"

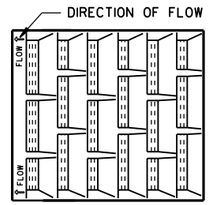
NOTE:
GRATE IS REVERSIBLE.

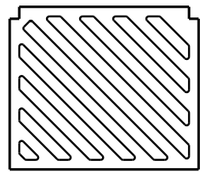
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

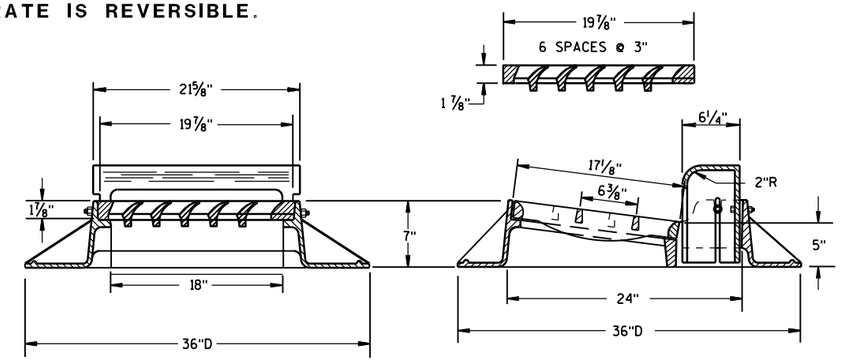




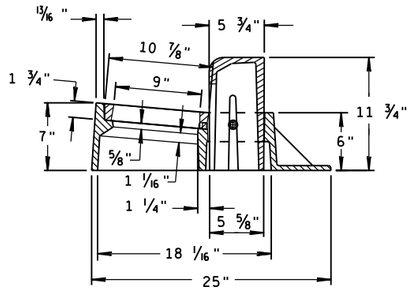
SPECIAL GRATE FOR TYPE "A" COVER

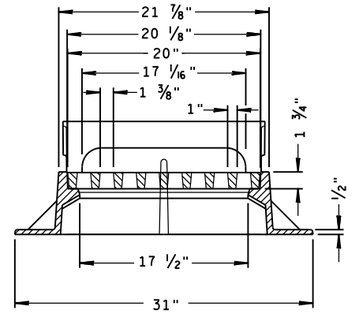
(MEASURES 19 3/4" X 17" X 1 7/8")

(NOTED AS TYPE A-S ON DRAINAGE TABLE)

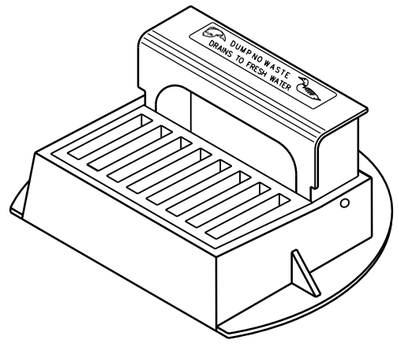


TYPE "A"





TYPE "Z"

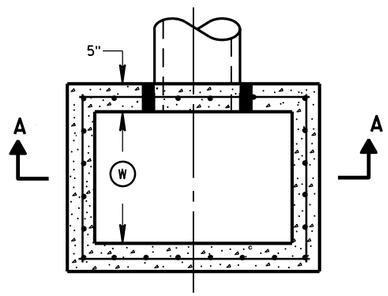


INLET COVERS
TYPE A, H, A-S, H-S & Z

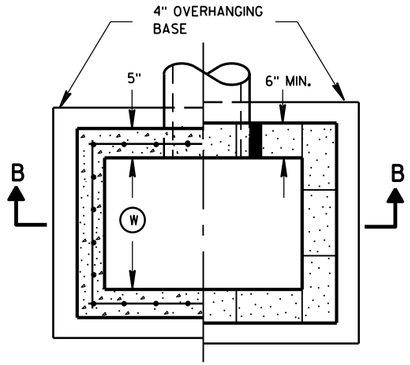
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11-27-13 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

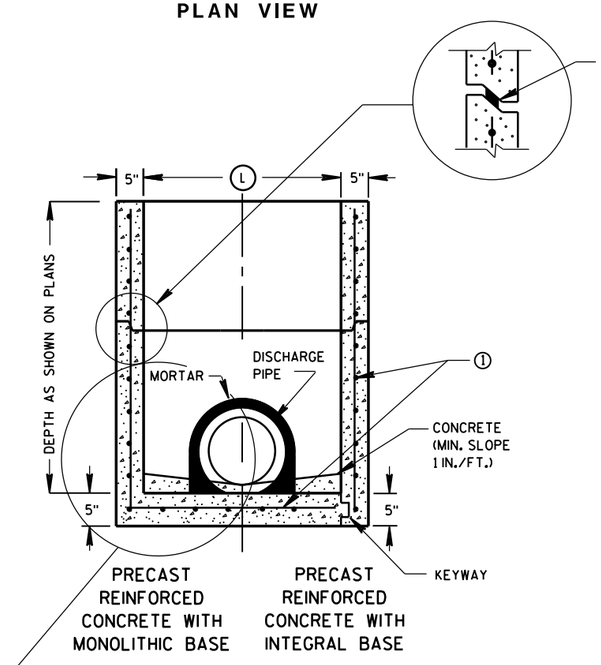
S.D.D. 8 A 5-19a



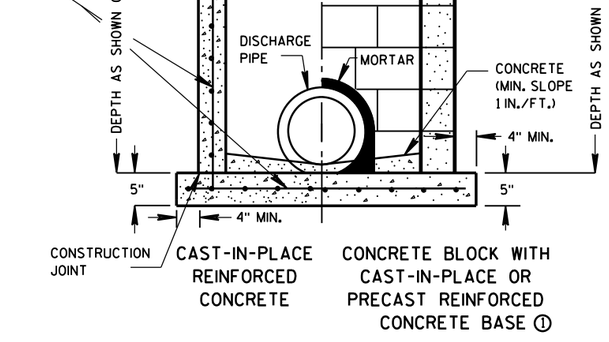
PLAN VIEW



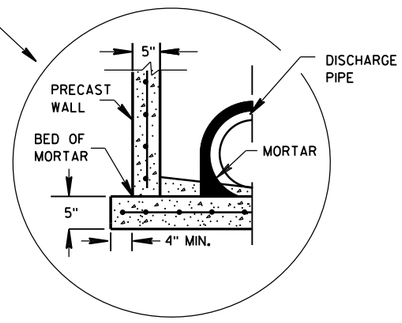
PLAN VIEW



SECTION A-A



SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

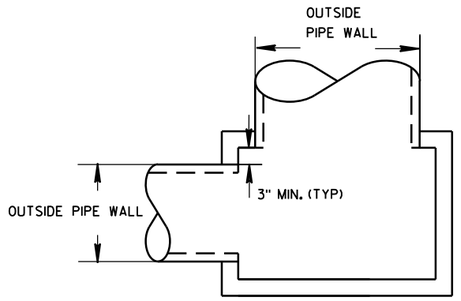
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH (W) (FT)	LENGTH (L) (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



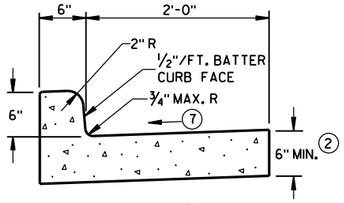
DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

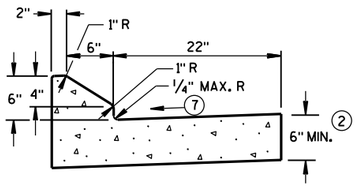
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2016
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS D IT
UNIT SUPERVIS
FHWA

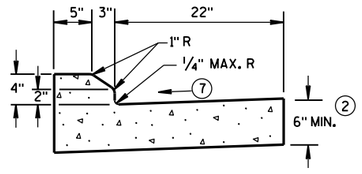
INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT



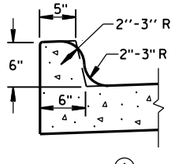
TYPES A (1) & D



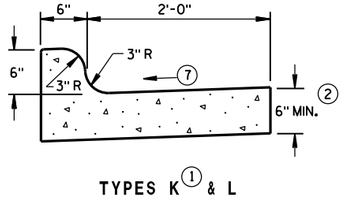
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

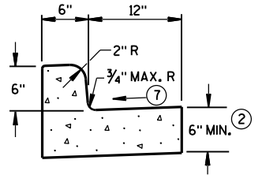


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



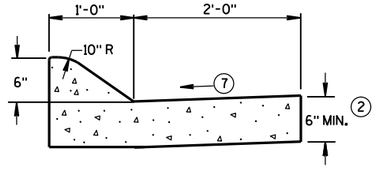
TYPES K (1) & L

CONCRETE CURB & GUTTER 30"

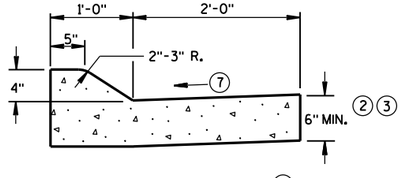


TYPES A (1) & D

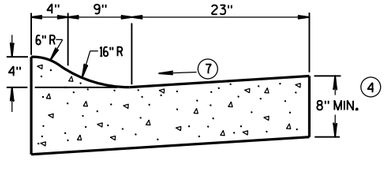
CONCRETE CURB & GUTTER 18"



6" SLOPED CURB TYPES A (1) & D

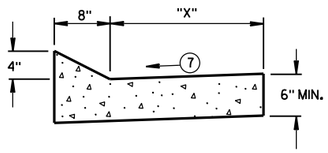


4" SLOPED CURB TYPES A (1) & D



4" SLOPED CURB TYPES R (1) & T (5)

CONCRETE CURB & GUTTER 36"



TYPES TBT & TBTT (1)

CONCRETE CURB & GUTTER

TBT & TBTT	"X"
30"	22"
36"	28"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

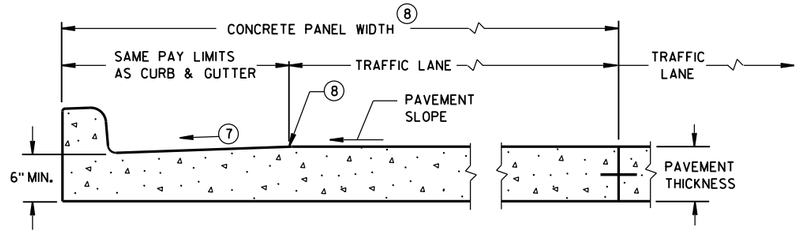
(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

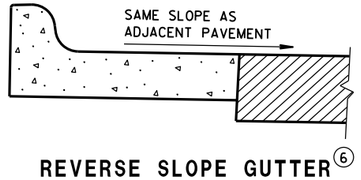
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB & GUTTER

* BIKE LANE IS NOT SHOWN.

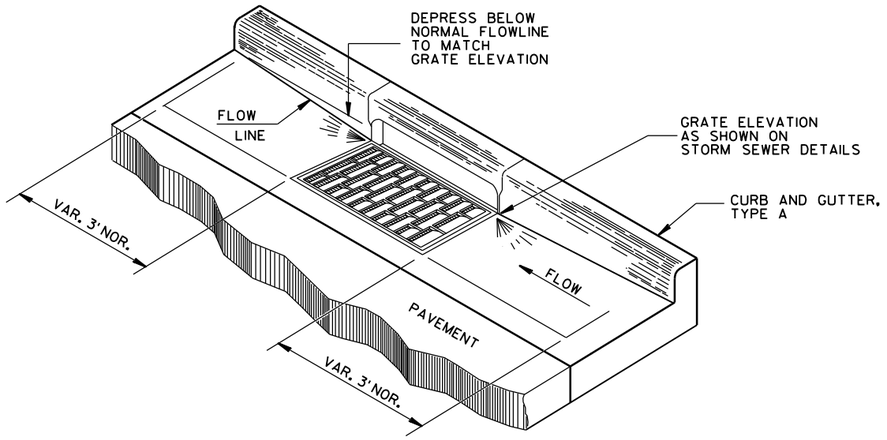


REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

CONCRETE CURB & GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

S.D.D. 8 D 1-20a



DETAIL OF CURB AND GUTTER AT INLETS

(TYPE H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

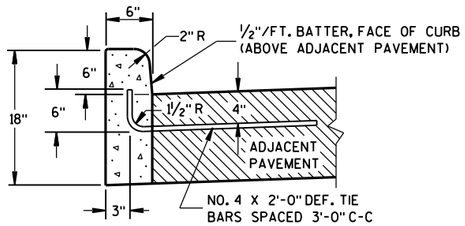
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

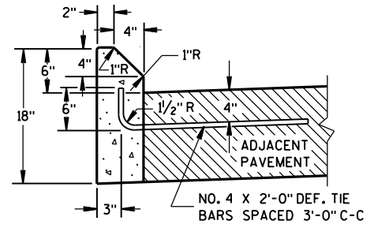
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(9) REFER TO SDD 8D18 AND SDD 8D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

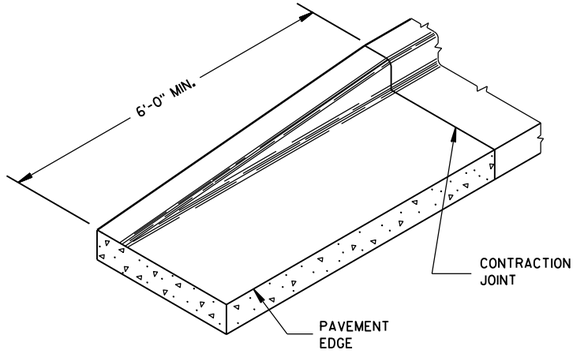


TYPES A(1) & D

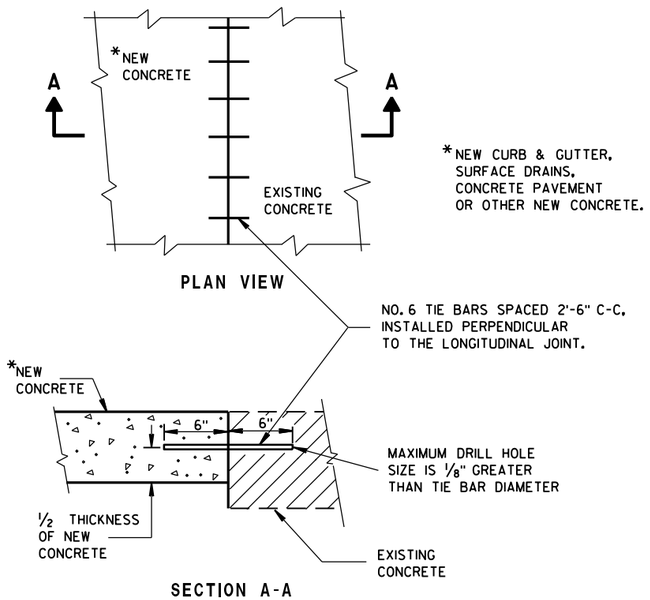


TYPES G(1) & J

CONCRETE CURB



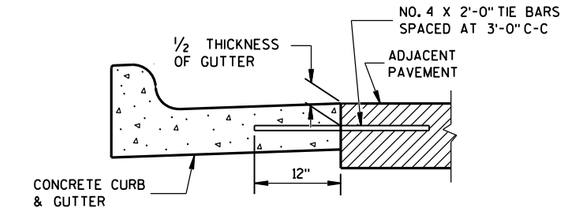
END SECTION CURB & GUTTER



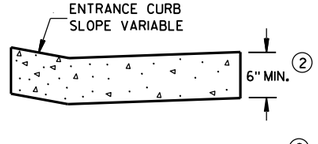
PLAN VIEW

SECTION A-A

TIE BARS DRILLED INTO EXISTING PAVEMENT



TYPICAL TIE BAR LOCATION (1)



DRIVEWAY ENTRANCE CURB (9)

(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2017 — DATE
/S/ Rodney Taylor
ROADWAY STANDARDS 37 — UNIT SUPERVISOR
ENT
FHWA

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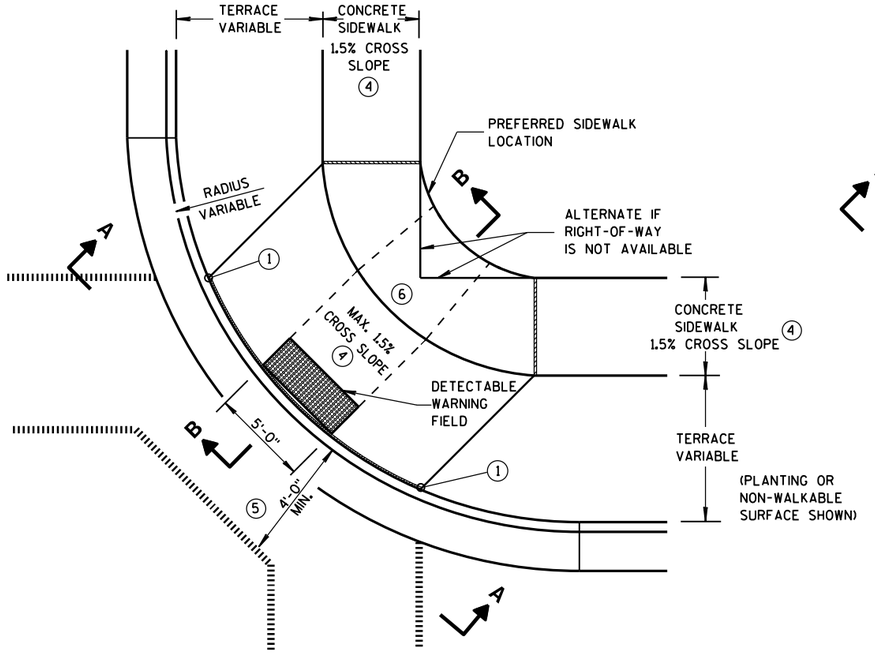
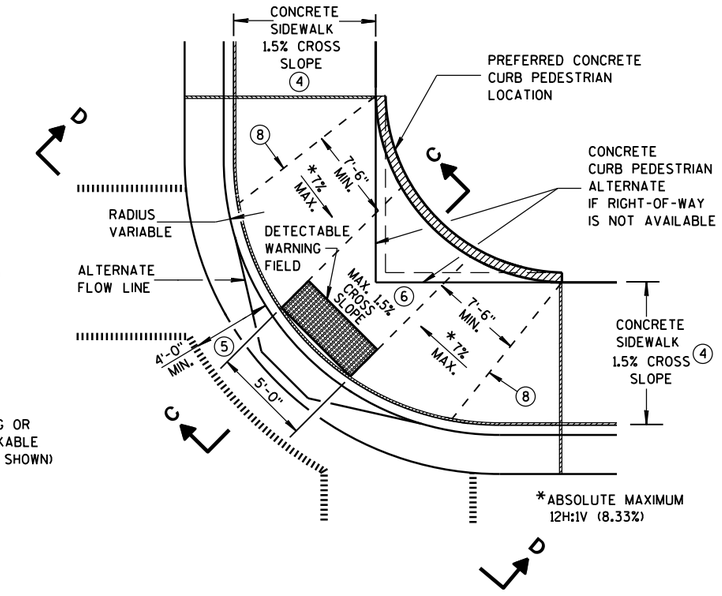
S.D.D. 8 D 1-20b



PLAN VIEW

TYPE 1 RAMP

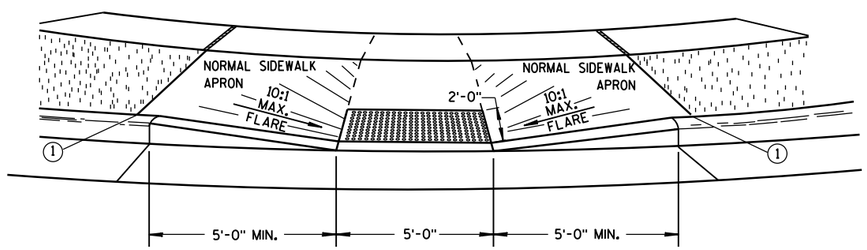
(CENTER OF CORNER RADIUS)



PLAN VIEW

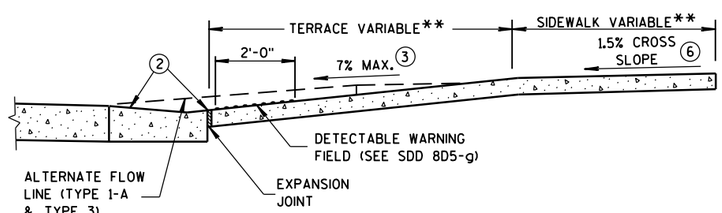
TYPE 1-A RAMP

(NO TERRACE)

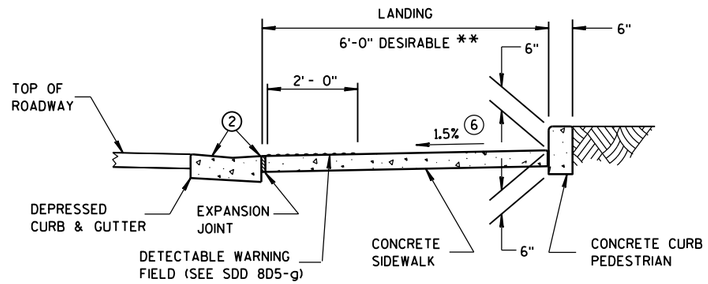


VIEW A-A

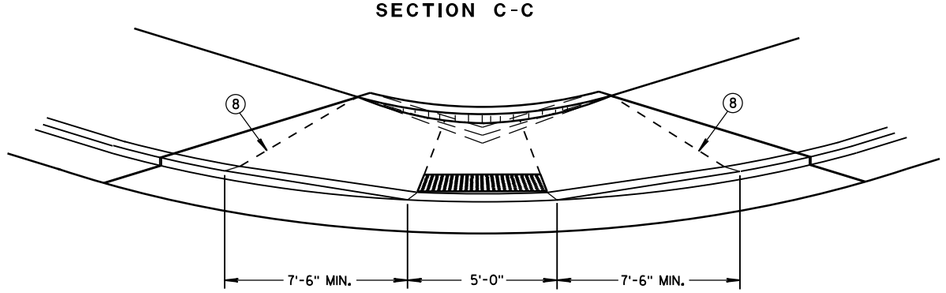
** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B-B



SECTION C-C



VIEW D-D

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

(1) THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(5) PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA. (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT

CURB RAMPS TYPES 1 AND 1-A

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DEPARTMENT OF TRANSPOI

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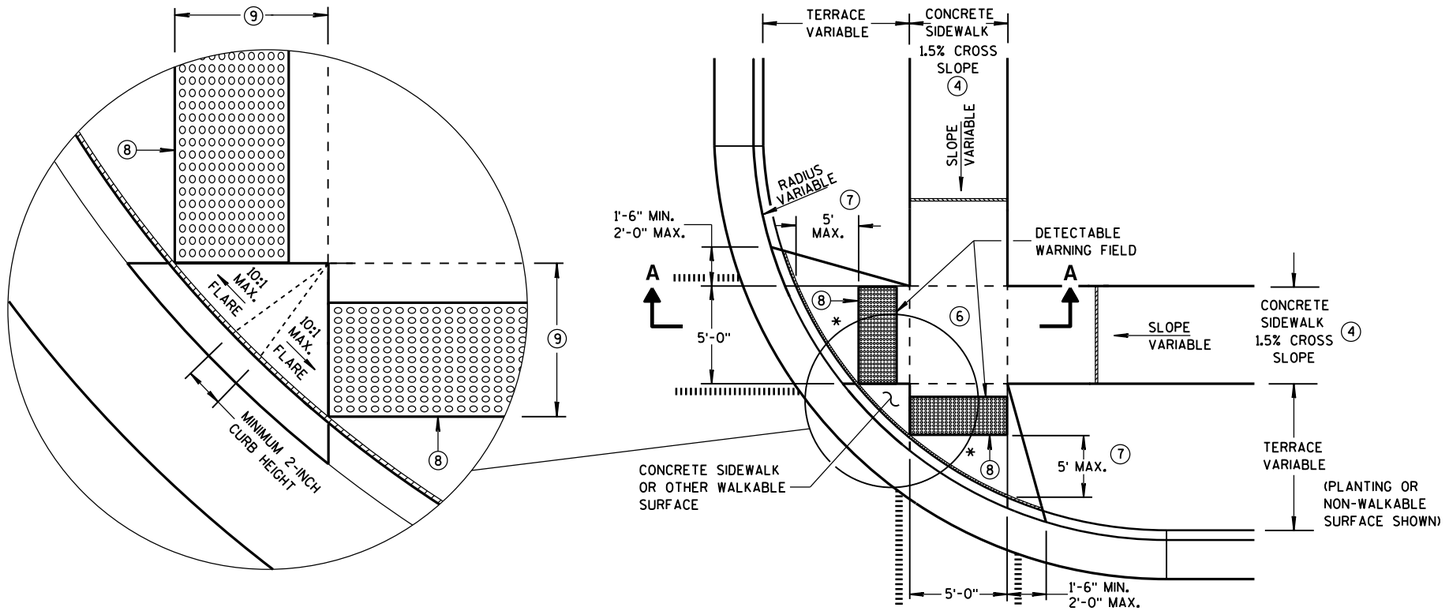
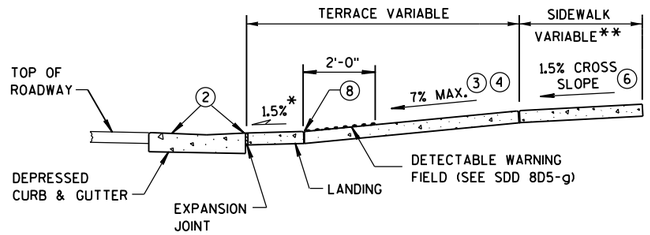


PLAN VIEW

TYPE 2 RAMP

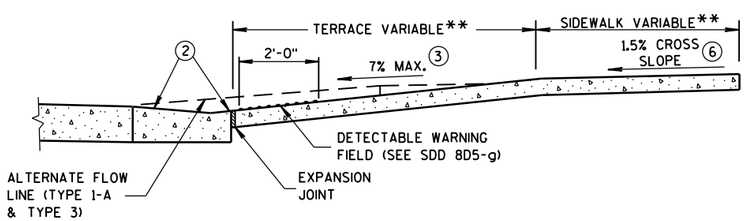
(ON LINE WITH SIDEWALK)

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

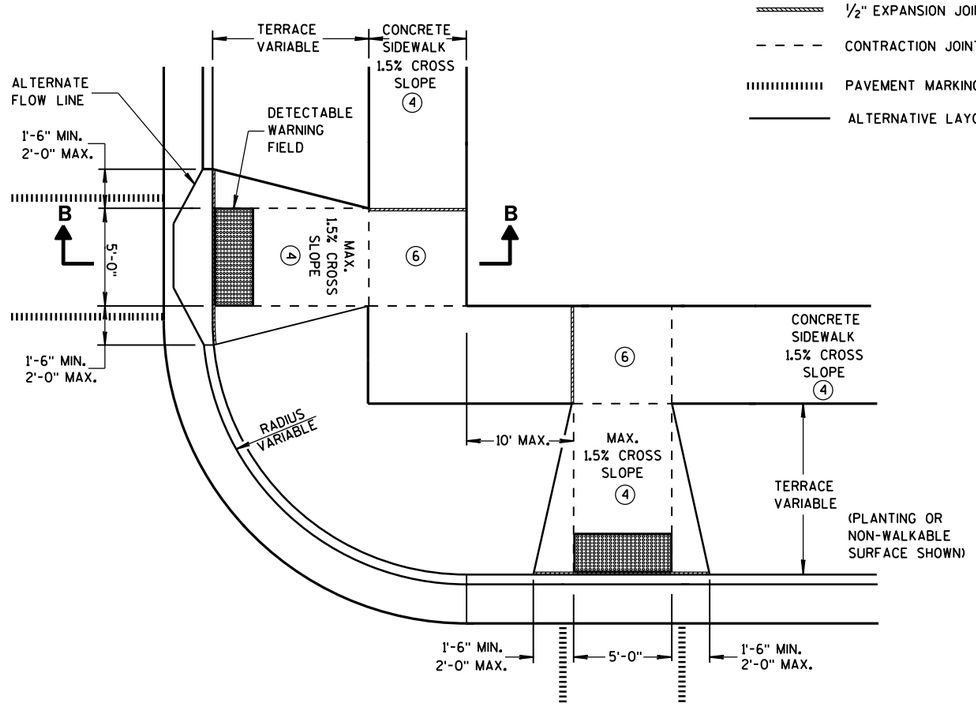


SECTION A-A

** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B-B



PLAN VIEW

TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(9) WHEN THIS DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

LEGEND

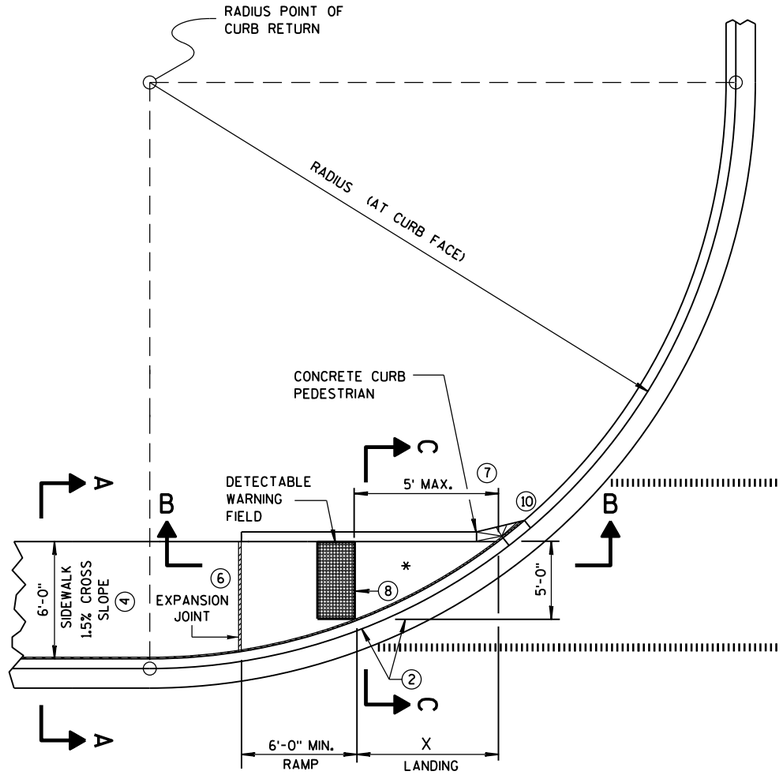
- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT

CURB RAMPS TYPES 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

6

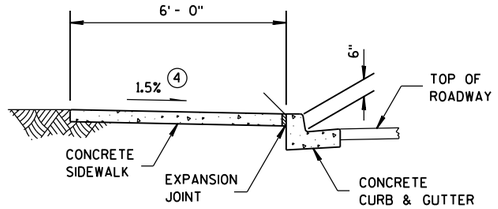
S.D.D. 8 D 5-19b



CURB RAMP TYPE 4A
PLAN VIEW

RADIUS (AT CURB FACE)	X
10 FEET	4'-7"
15 FEET	6'-5½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

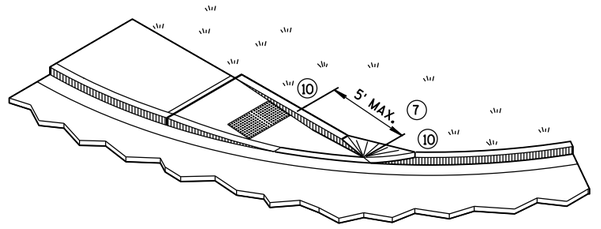
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

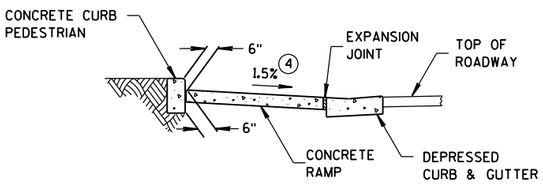
(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(10) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

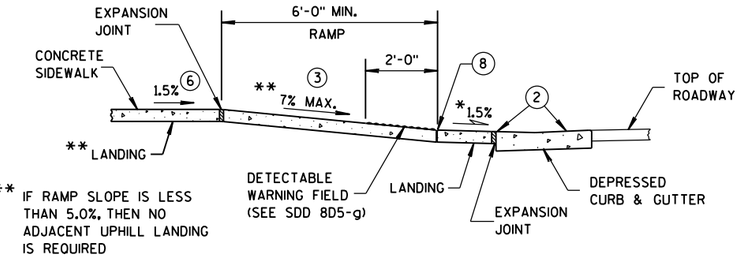


ISOMETRIC VIEW FOR TYPE 4A



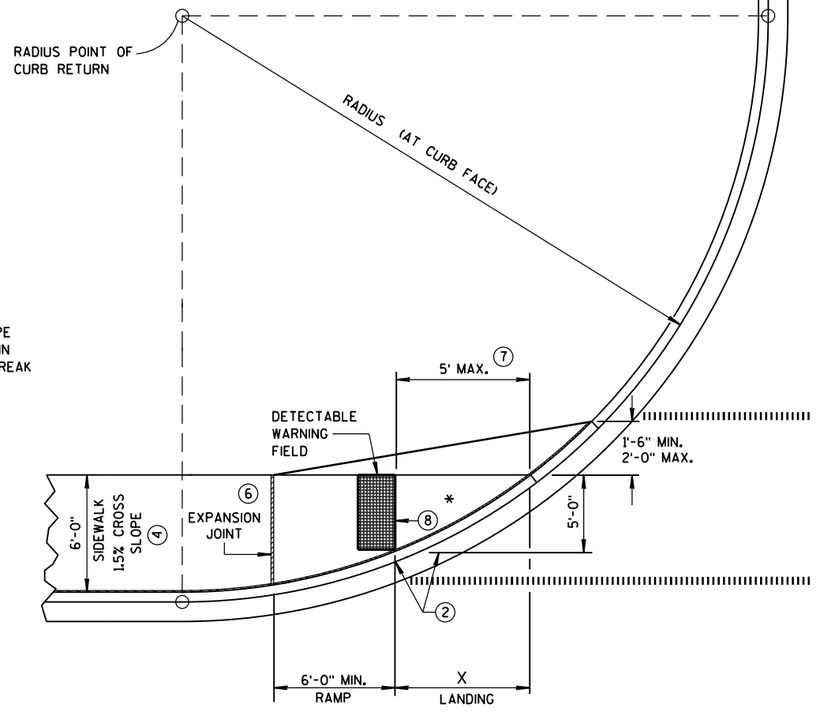
SECTION C-C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

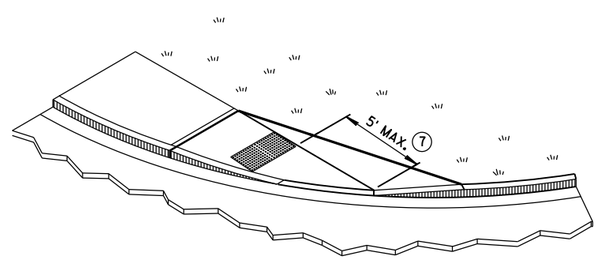


** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

SECTION B-B FOR TYPE 4A



CURB RAMP TYPE 4A1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

½" EXPANSION JOINT-SIDEWALK

CONTRACTION JOINT FIELD LOCATED

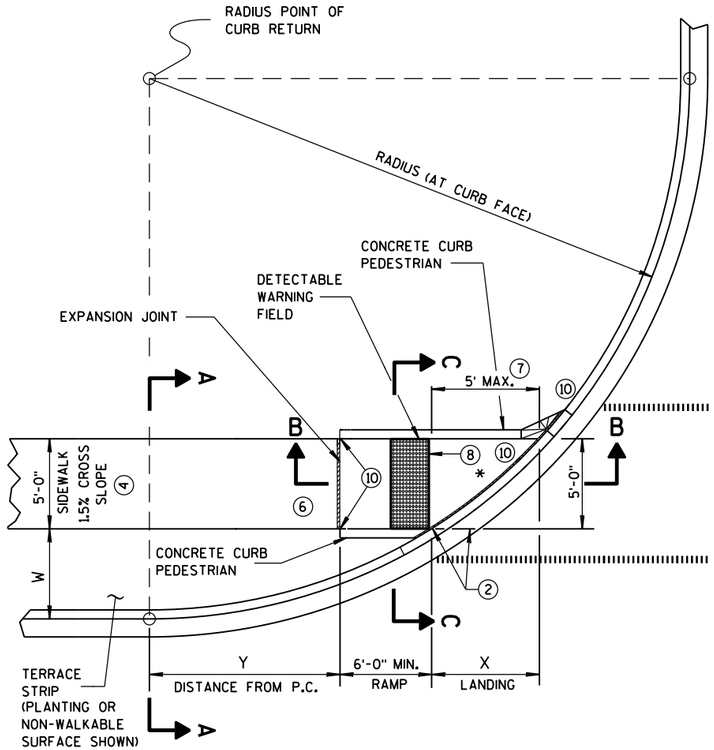
PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 4A AND 4A1

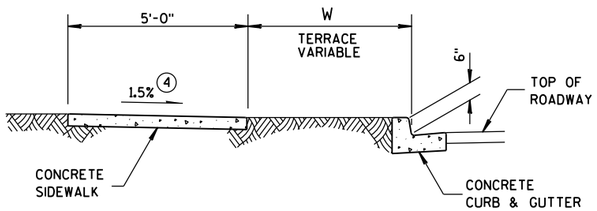
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

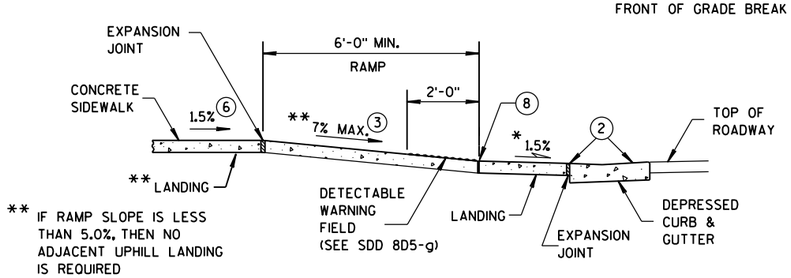
S.D.D. 8 D 5-19c



CURB RAMP TYPE 4B
PLAN VIEW



SECTION A-A FOR TYPE 4B



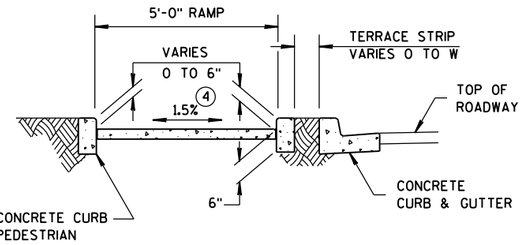
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

SECTION B-B FOR TYPE 4B

RADIUS (AT CURB FACE)	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"		W = 8'-0"		W = 9'-0"		W = 10'-0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2'-10¼"	0'-5"	2'-1"	1'-4½"	1'-5"	2'-1"	0'-10"	2'-7½"	0'-3¼"	3'-0¼"						
15 FEET	4'-6¾"	2'-1¾"	3'-9"	3'-5¼"	3'-1¼"	4'-6"	2'-6¾"	5'-4½"	2'-1"	6'-1"	1'-8"	6'-8½"	1'-3¼"	7'-2½"	0'-10¾"	7'-7¼"
20 FEET	5'-9¾"	3'-6½"	4'-11½"	5'-1¾"	4'-3¼"	6'-5½"	3'-8¾"	7'-7"	3'-3"	8'-6½"	2'-10"	9'-4½"	2'-5½"	10'-1¼"	2'-1¼"	10'-9"
30 FEET			6'-9¼"	7'-11¼"	6'-0¼"	9'-8"	5'-5"	11'-1¾"	4'-10¾"	12'-5¾"	4'-5½"	13'-7¾"	4'-0¾"	14'-8½"	3'-8½"	15'-8¼"
40 FEET									6'-1¾"	15'-8½"	5'-8"	17'-2"	5'-3"	18'-5¾"	4'-10¾"	19'-8¼"
50 FEET															5'-10¼"	23'-2"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C-C FOR TYPE 4B

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

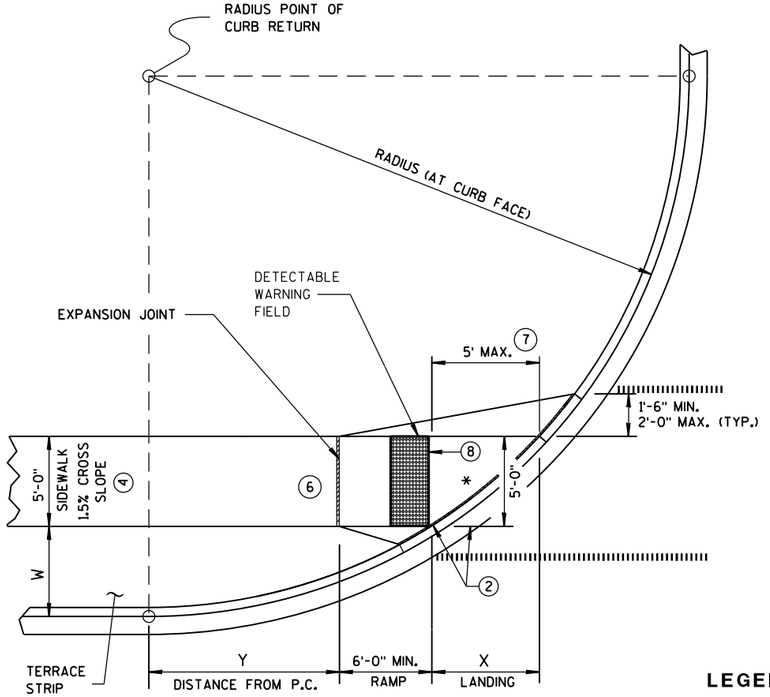
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

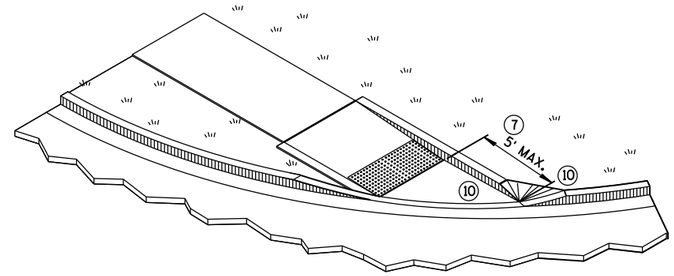
(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

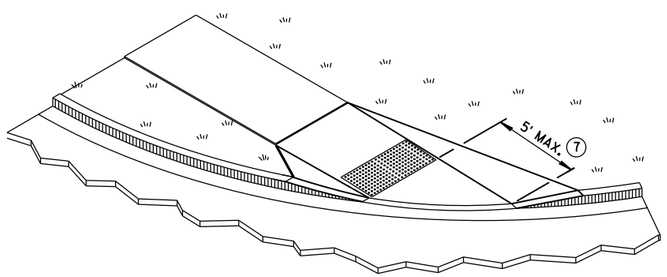
(10) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



CURB RAMP TYPE 4B1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

LEGEND

½" EXPANSION JOINT-SIDEWALK

CONTRACTION JOINT FIELD LOCATED

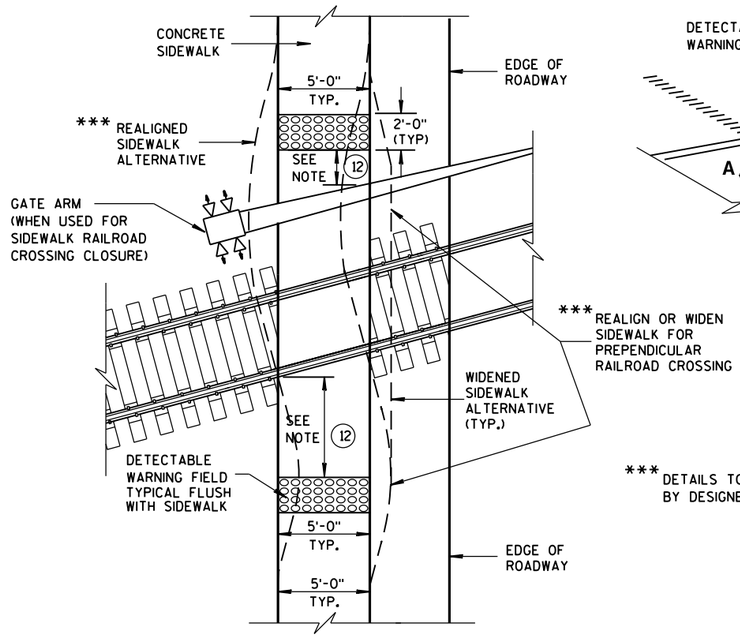
PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 4B AND 4B1

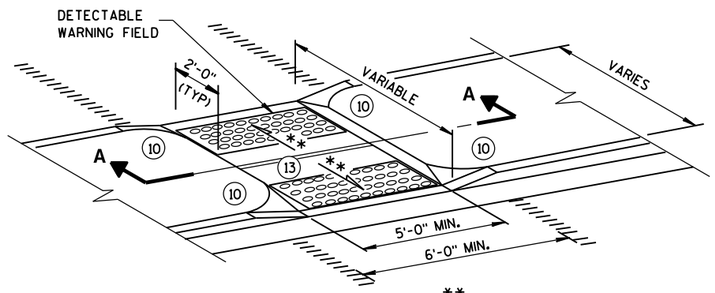
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 8 D 5-19d



TYPE 8
DETECTABLE WARNINGS AT RAILROAD CROSSING



** 1% MIN. (PROVIDE DRAINAGE)

*** DETAILS TO BE DETERMINED BY DESIGNER

MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING
TYPE 5

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

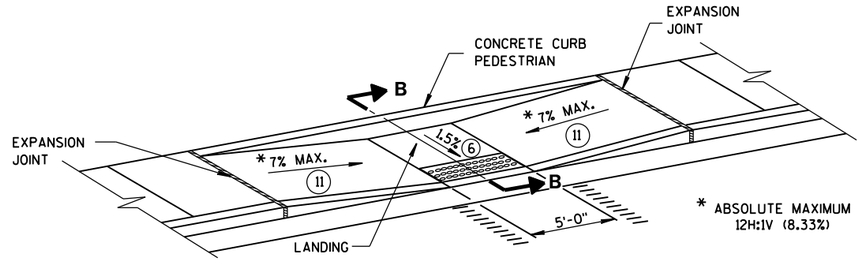
(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

(10) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

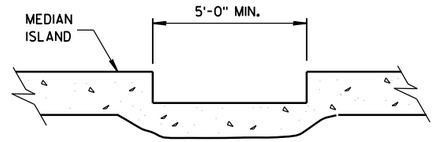
(11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

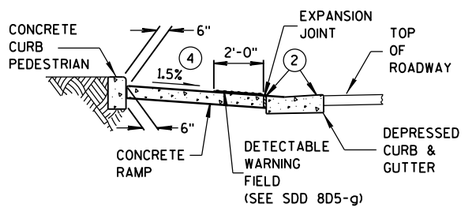
(13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2-FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.



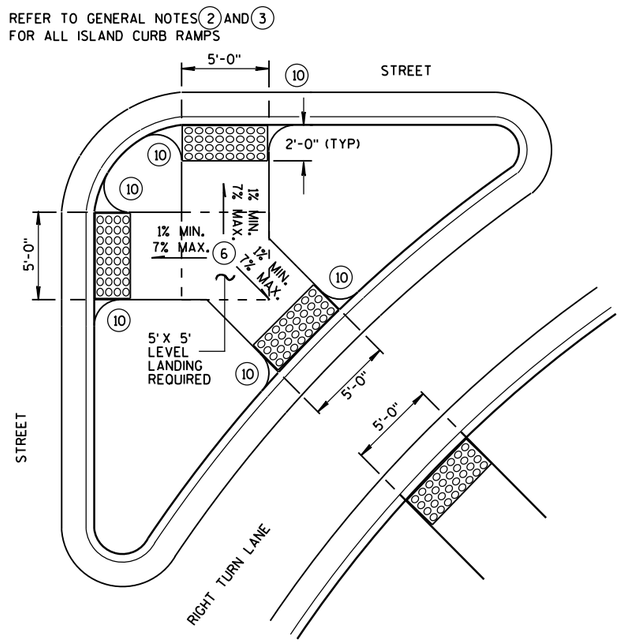
MID-BLOCK CROSSING
TYPE 7A



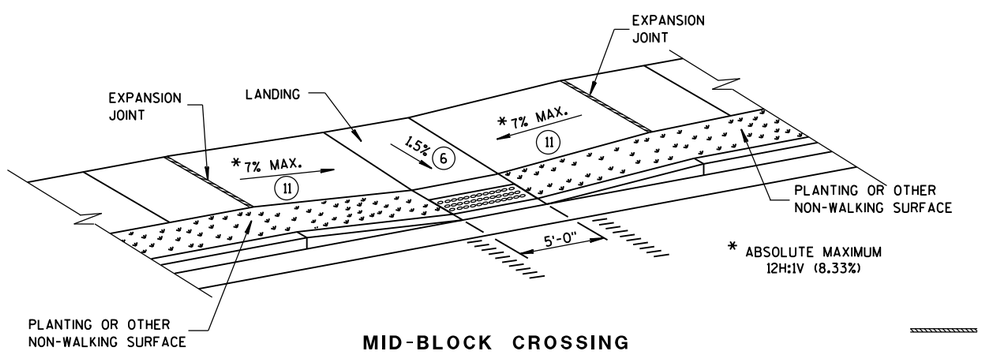
SECTION A-A



SECTION B-B



TYPE 6
DETECTABLE WARNING AT ISLANDS



MID-BLOCK CROSSING
TYPE 7B

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

LEGEND

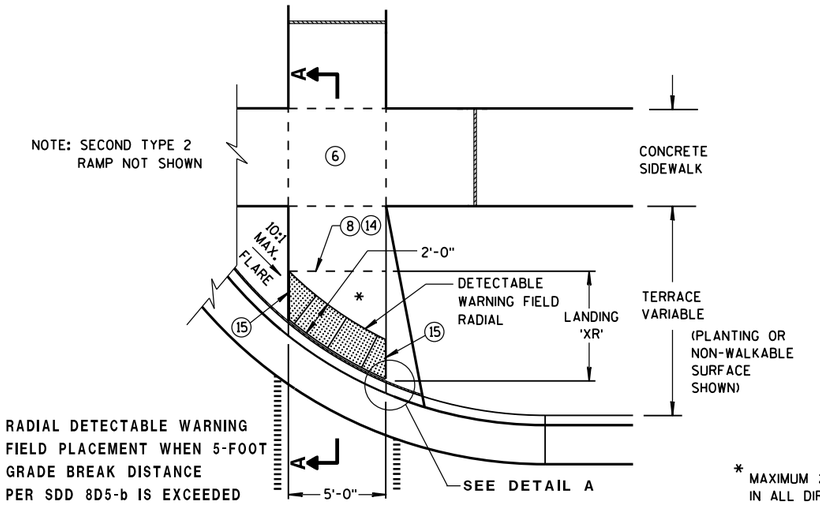
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

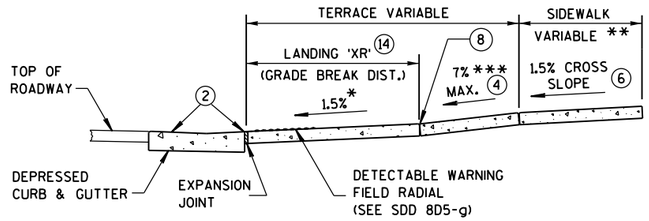
6

S.D.D. 8 D 5-19e



TYPE 2 RAMP
PLAN VIEW
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK



** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** ABSOLUTE MAXIMUM 12H:1V (8.33%)

SECTION A-A

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS (DWFs) THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.

REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

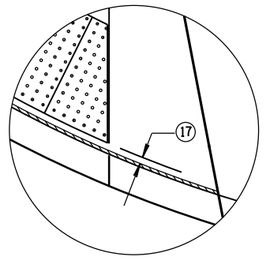
(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION 'XR') REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.

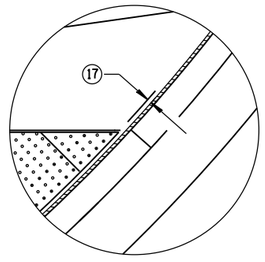
(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

(16) USE 1'X 2' RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2'-0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.

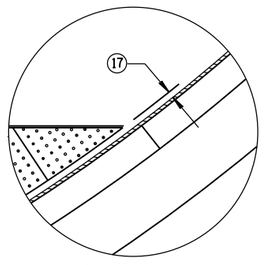
(17) A MAXIMUM 3-INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.



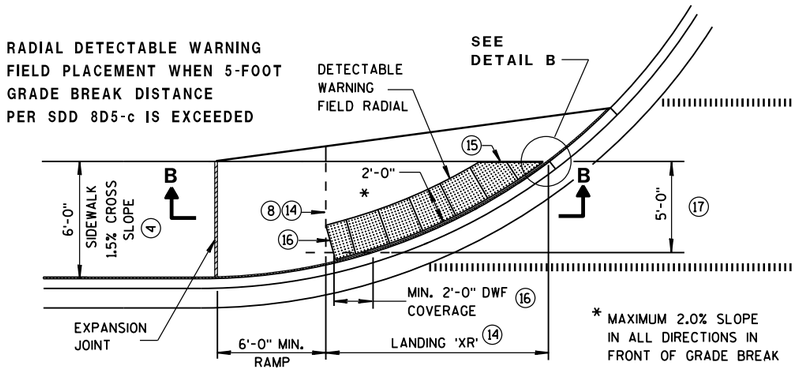
DETAIL A



DETAIL C

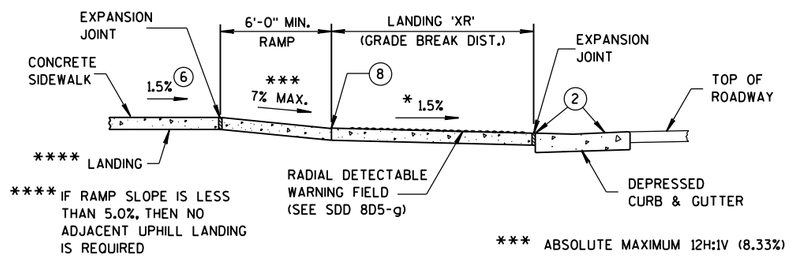


DETAIL B



* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

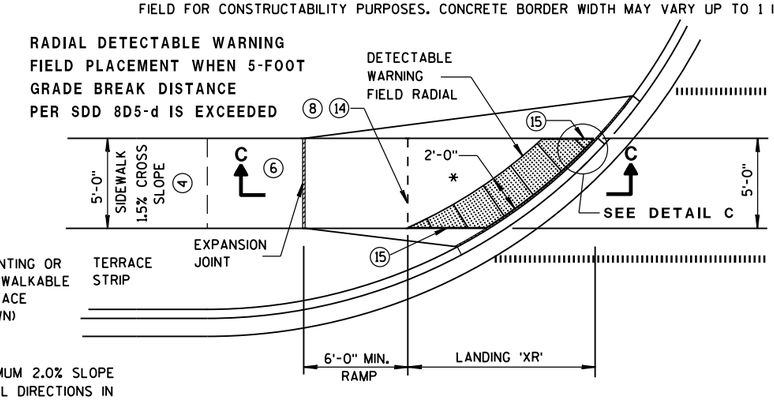
CURB RAMP TYPE 4A1
PLAN VIEW
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

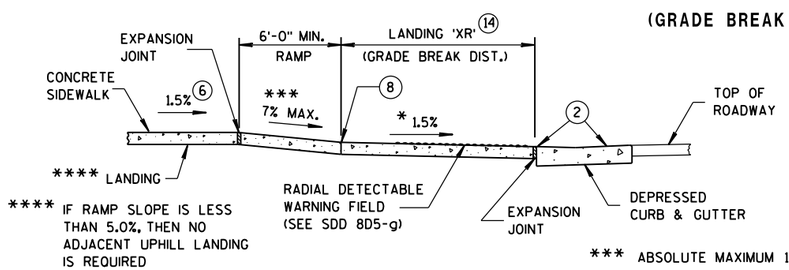
*** ABSOLUTE MAXIMUM 12H:1V (8.33%)

SECTION B-B FOR TYPE 4A1



* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

CURB RAMP TYPE 4B1
PLAN VIEW
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** ABSOLUTE MAXIMUM 12H:1V (8.33%)

SECTION C-C FOR TYPE 4B1

CURB RAMPS
RADIAL DETECTABLE WARNING FIELD APPLICATIONS

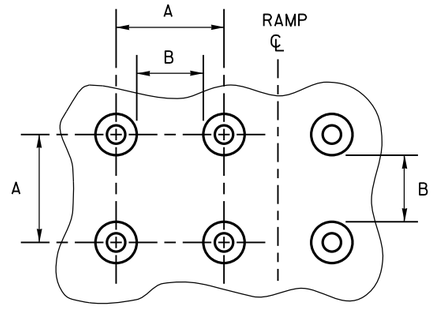
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

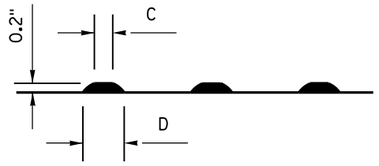
S.D.D. 8 D 5-19f

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



PLAN VIEW



ELEVATION VIEW

TRUNCATED DOMES

DETECTABLE WARNING PATTERN DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

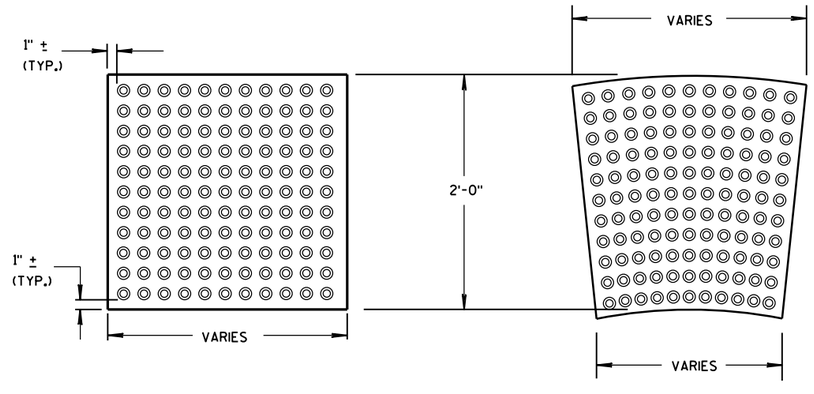
DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGES IN COMBINATION WITH SQUARE PANELS ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

⑮ FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

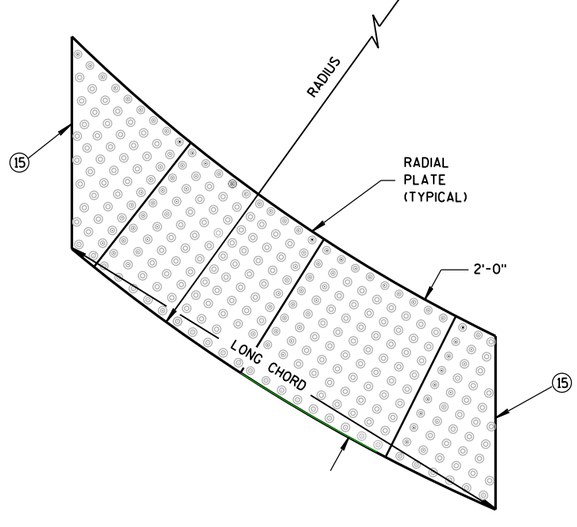


RECTANGULAR PLATES

RADIAL PLATES

DETECTABLE WARNING FIELDS (TYPICAL)

PLAN VIEW



RADIAL DETECTABLE WARNING FIELD ATTRIBUTES

CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2017 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS UNIT SUPERV
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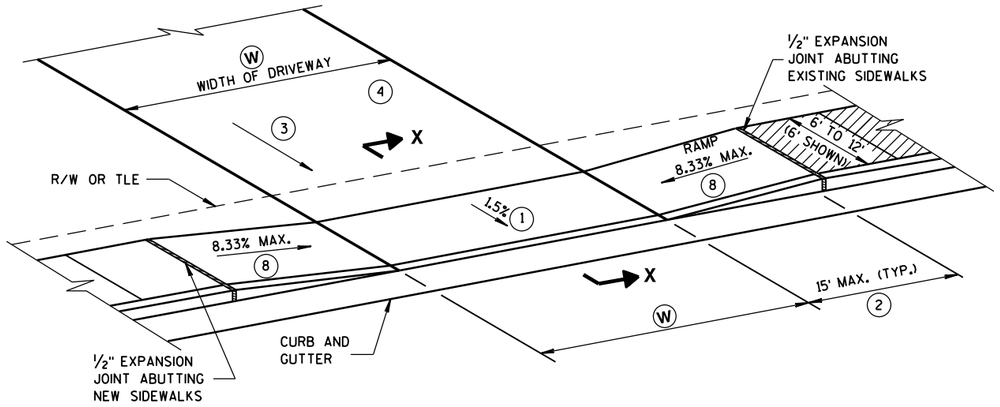
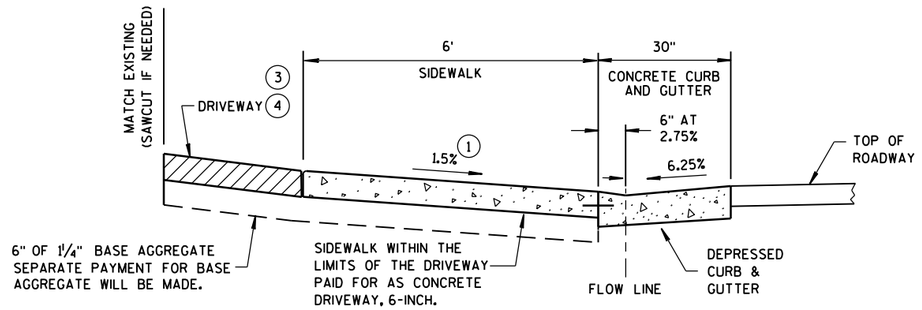
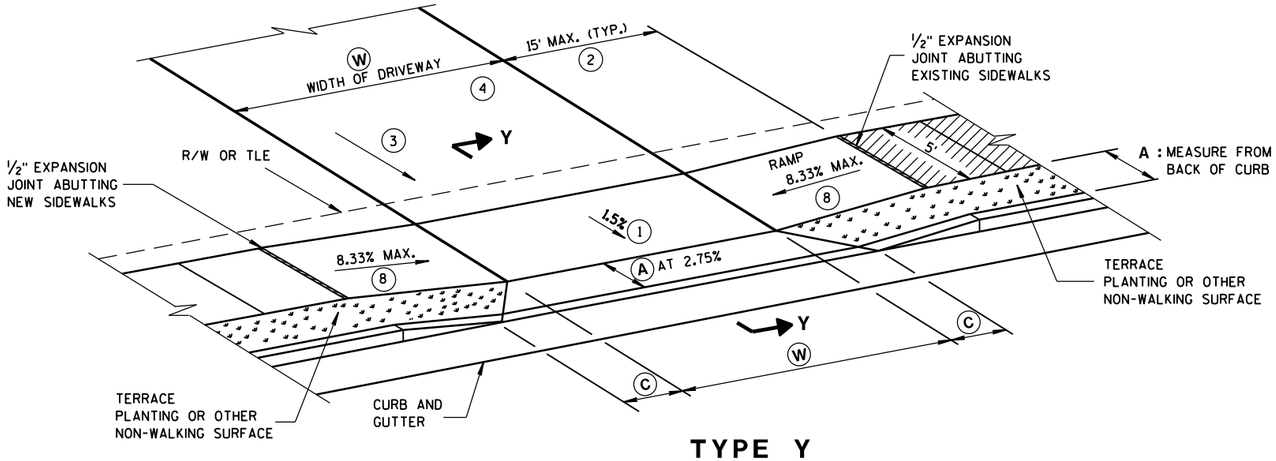


TYPE X
SIDEWALK ABUTS CURB & GUTTER
TERRACE VARIES 0 TO 3 FEET



SECTION X-X

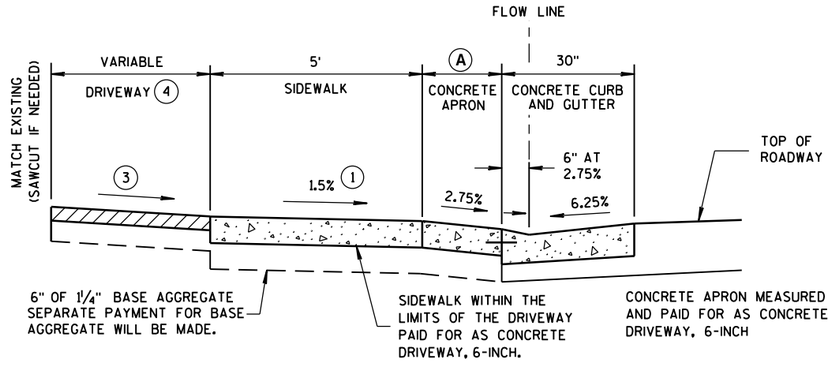


TYPE Y
SIDEWALK WITH NARROWER TERRACE
TERRACE VARIES 4 TO 6 FEET

W: 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)

GENERAL NOTES

1. CONSTRUCTION TOLERANCE OF 0.5% ± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
2. THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
3. DRIVEWAY SLOPES: DESIRABLE MAXIMUM
   - 10.5% UP AWAY FROM SIDEWALK (SAG)
   - 8.5% DOWN AWAY FROM SIDEWALK (CREST)
   - ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
4. DRIVEWAY TYPES
   - 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
   - 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
   - 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES)
5. PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.
6. (W) IS SHOWN ON PLAN AND PROFILE SHEETS.
7. OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.
8. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.



NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS

SECTION Y-Y

DRIVEWAY DETAIL
WITH CONCRETE CURB & GUTTER
(URBAN AND SUBURBAN)

TABLE Y

A FEET	C FEET
3.5'	2.0'
4.5'	3.0'
5.5'	3.5'

NOT TO SCALE

DRIVEWAY AND SIDEWALK RAMPS
TYPES X & Y

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December, 2016 — /S/ Rodney Taylor
DATE — ROADWAY STANDARDS UNIT SUPERV — ENT
FHWA

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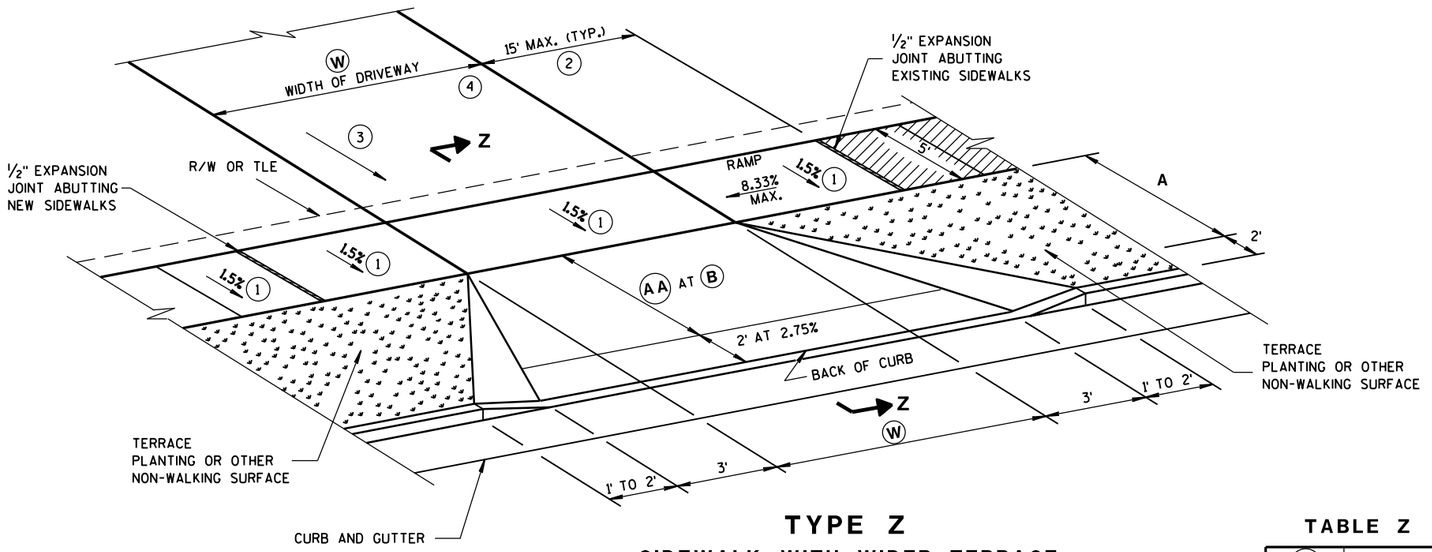


TYPE Z

SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET

TABLE Z

(AA) FEET	(B) %
4.5'	11.5%
5.5'	9-11.5%
6.5'	8-11.5%
7.5'	7-11.5%
8.5'	6-11.5%
9.5'	5-11.5%

GENERAL NOTES

(1) CONSTRUCTION TOLERANCE OF 0.5% ± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

(2) THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.

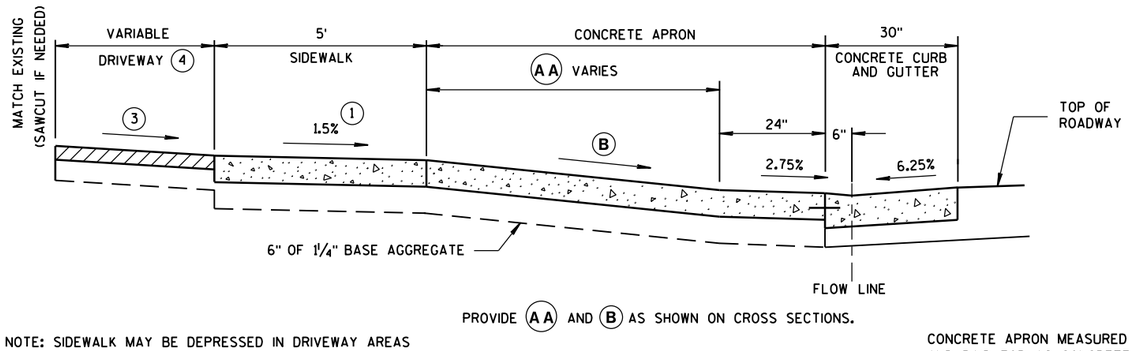
(3) DRIVEWAY SLOPES: DESIRABLE MAXIMUM
- 10.5% UP AWAY FROM SIDEWALK (SAG)
- 8.5% DOWN AWAY FROM SIDEWALK (CREST)
- ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG

(4) DRIVEWAY TYPES
- 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
- 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
- 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES)

(5) PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(6) (W) IS SHOWN ON PLAN AND PROFILE SHEETS.

(7) OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.



PROVIDE (AA) AND (B) AS SHOWN ON CROSS SECTIONS.

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR **B** VALUES NOT SHOWN IN TABLE Z.

SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY, 6-INCH.

SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE.

CONCRETE APRON MEASURED AND PAID FOR AS CONCRETE DRIVEWAY, 6-INCH

SECTION Z-Z

DRIVEWAY DETAIL
WITH CONCRETE CURB & GUTTER

(URBAN AND SUBURBAN)

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)

NOT TO SCALE

DRIVEWAY AND SIDEWALK RAMPS TYPE Z

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December, 2016 — /S/ Rodney Taylor
DATE — ROADWAY STANDARDS UNIT SUPERV — ENT
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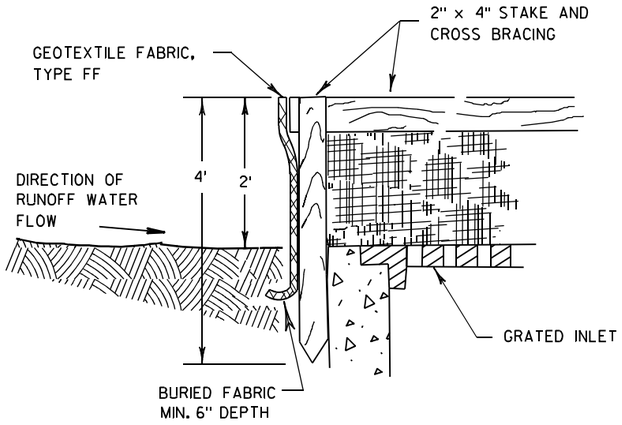
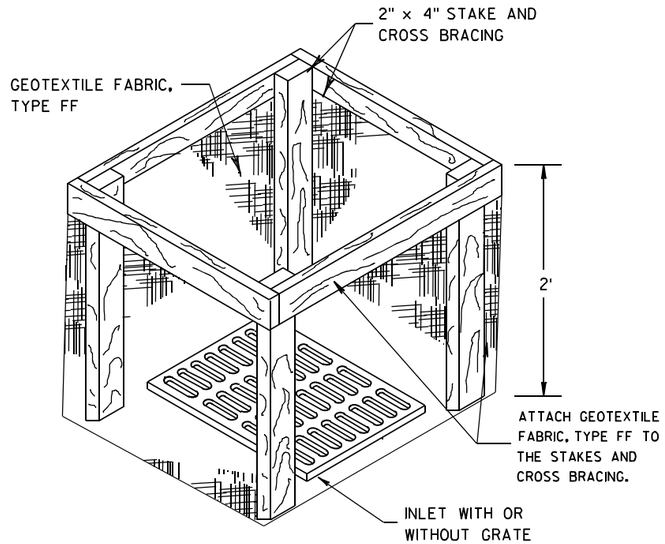
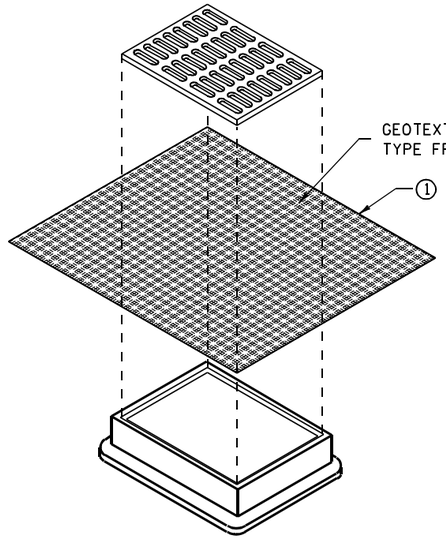


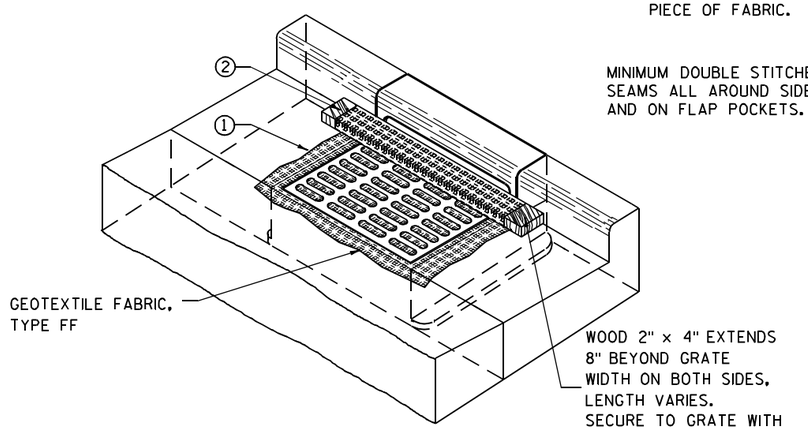


INLET PROTECTION, TYPE A

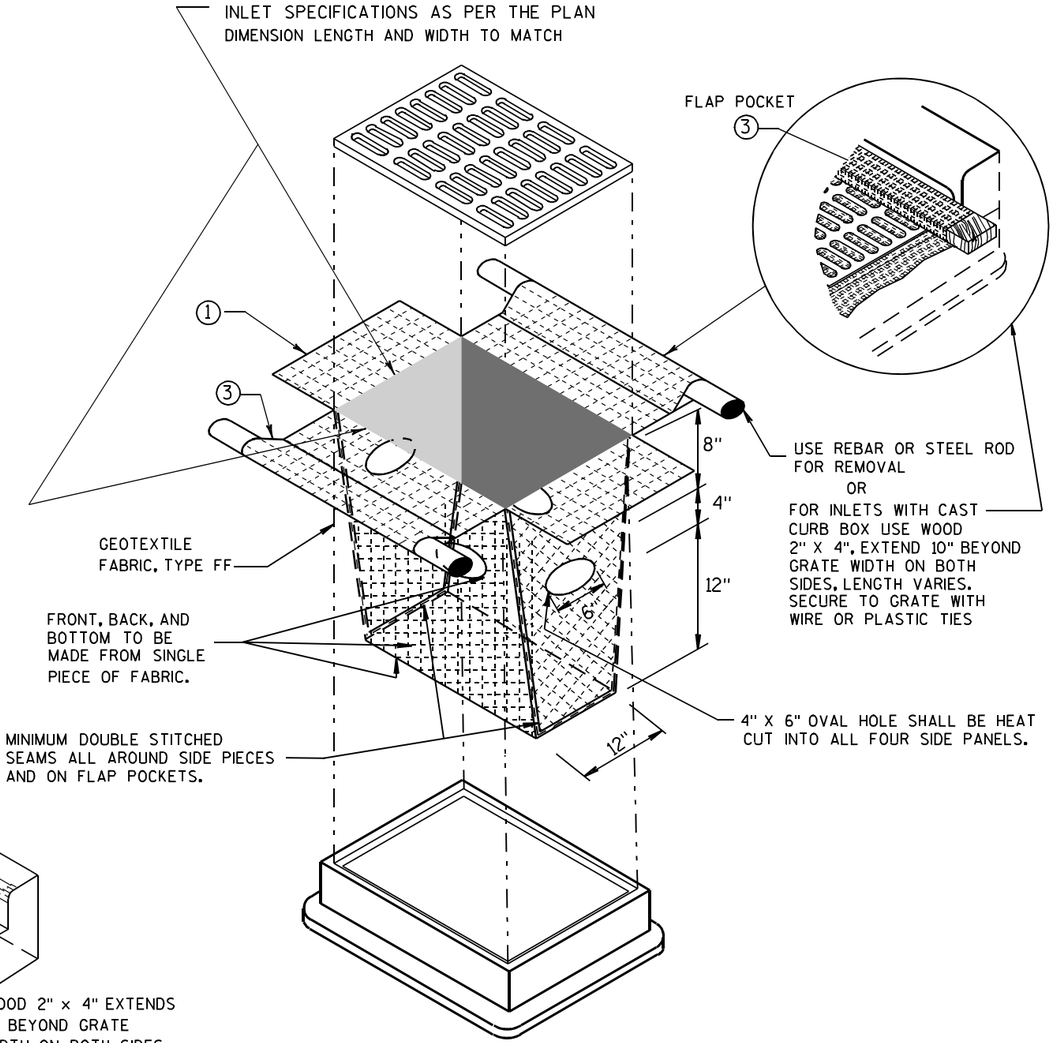


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

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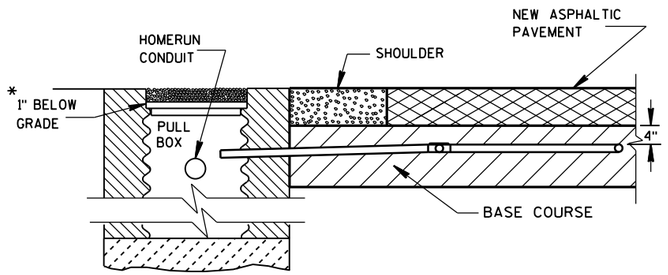
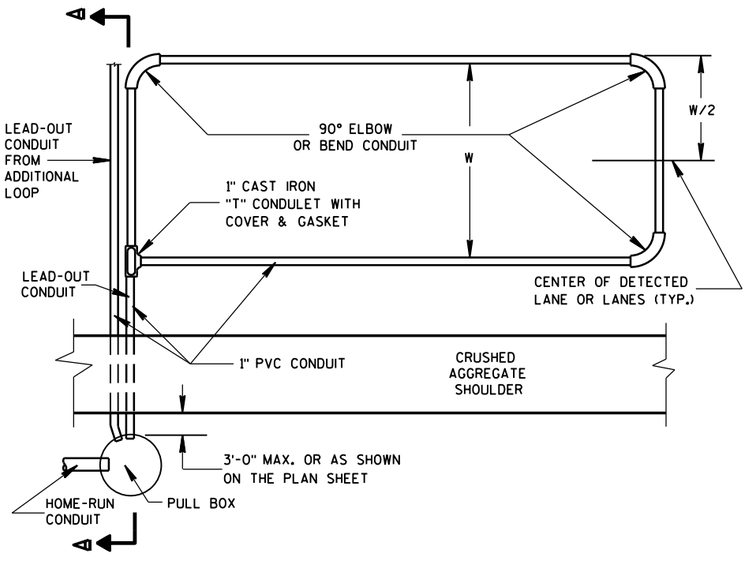


SECTION A-A
NO CURB & GUTTER

DETECTOR LOOP INSTALLATION DETAIL

*RECESS PULL BOX SO THAT THE COVER IS 3" BELOW GRADE IN SHOULDER AREAS OF CRUSHED AGGREGATE. BACKFILL OVER COVER WITH THE CRUSHED AGGREGATE TO BRING THE AREA TO GRADE LEVEL.



TYPICAL PLAN OF LOOP DETECTOR

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

LOOP SIZE, LOCATION, NUMBER OF TURNS OF WIRE AND ASSOCIATED SIGNAL PHASE SHALL BE AS SHOWN ON THE PLANS.

PITCH LEAD-OUT CONDUIT TO DRAIN TO ROADSIDE PULL BOX.

SPLICES SHALL BE INSTALLED BY USING CAST IN PLACE SPLICE KITS LISTED ON THE DEPARTMENTS APPROVED PRODUCTS LIST OR AN ENGINEER APPROVED EQUAL. NON-INSULATED BUTT SPLICES TO FIT #12 AWG STRANDED WIRE SHALL BE USED. SPLICES SHALL BE SOLDERED AND INSULATED FROM EACH OTHER AS PER INSTRUCTIONS INCLUDED IN THE SPLICE KIT.

MEASURE GROUND RESISTANCE USING A MEGGER. REPLACE LOOP WIRE NOT ATTAINING A READING OF INFINITY TO GROUND.

AFTER SPLICING THE LOOP WIRE TO THE LOOP LEAD-IN CABLE, THE CONTRACTOR SHALL MEASURE INDUCTANCE, GROUND RESISTANCE AND WIRE RESISTANCE AT THE CABINET END OF THE LEAD-IN CABLE AND FURNISH A COPY OF THE READINGS TO THE PROJECT ENGINEER FOR EVALUATION.

LOOP DETECTOR LEADS SHALL BE IDENTIFIED WITH THEIR ASSOCIATED LOOP BY USE OF WATERPROOF TAGS AT BOTH ENDS OF THE CABLE. A LISTING OF THE CABLE IDENTIFICATION PER INDIVIDUAL LOOP LEAD-IN SHALL BE PLACED IN THE CABINET.

THE #12 AWG LOOP WIRE FROM THE LOOP TO THE ROADSIDE PULL BOX, SHALL BE HAND TWISTED AT LEAST 3 TWISTS PER FOOT BEFORE INSTALLATION.

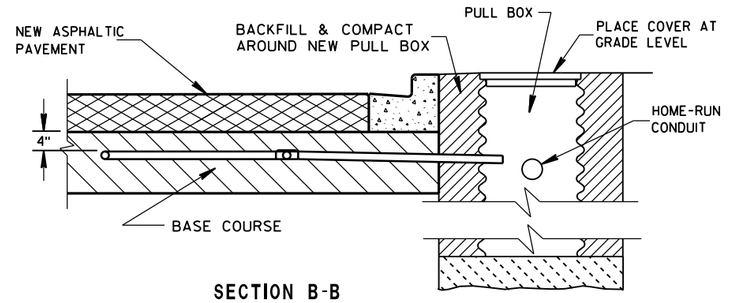
SPLICES OF LOOP WIRE TO LEAD-IN CABLE SHALL BE MADE ONLY IN PULL BOXES AT THE SIDE OF THE ROAD.

THE #12 AWG LOOP WIRE SHALL BE INSTALLED FROM THE ROADSIDE PULL BOX, THROUGH THE LOOP DUCT, BACK TO THE ROADSIDE PULL BOX, AND BE INSTALLED IN ONE, NON-SPLICED, CONTINUOUS LENGTH.

PROTECTION OF THE CONDUIT AND CONDULET SHALL BE REQUIRED AFTER INSTALLATION AND BEFORE THE ASPHALTIC PAVEMENT IS PLACED.

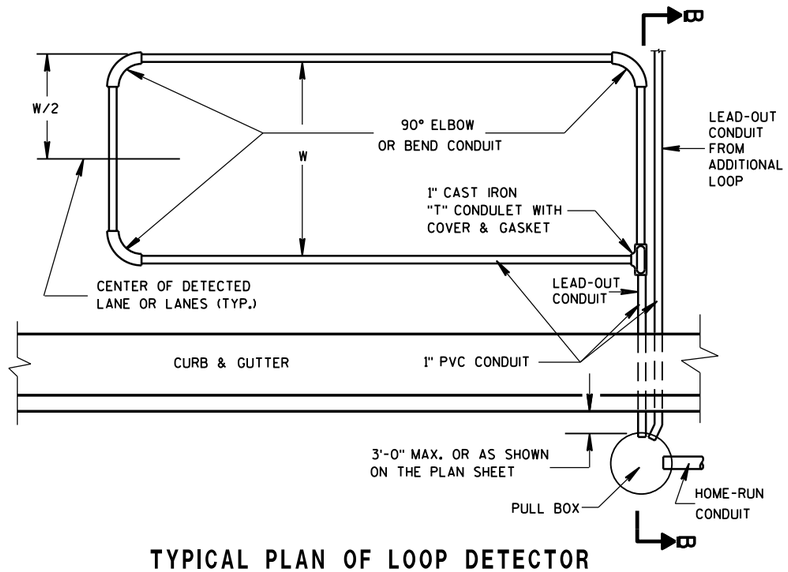
WHEN MULTIPLE LAYERS OF ASPHALTIC PAVEMENT ARE TO BE PLACED, LOOPS MAY BE INSTALLED BY SAWING A TWO INCH WIDE SLOT IN THE FIRST LAYER, DIG OUT THE ASPHALTIC MATERIAL AND BASE COURSE, PLACE THE LOOP, FILL THE SLOT WITH BASE COURSE MATERIAL AND NEW ASPHALTIC MATERIAL AND TAMP THE ASPHALTIC MATERIAL IN PLACE.

SHOULD TRAFFIC BE ALLOWED TO USE THE AREA OF ROADWAY WITH THE NEWLY INSTALLED LOOP BEFORE THE PLACEMENT OF THE NEXT LAYER OF ASPHALTIC PAVEMENT, THE SLOT/PAVEMENT OPENING SHALL BE SEALED WITH HOT POURED ELASTIC TYPE MATERIAL CONFORMING TO THE REQUIREMENTS OF THE "SPECIFICATION FOR JOINT SEALANTS, HOT POURED, FOR CONCRETE AND ASPHALT PAVEMENTS, ASTM DESIGNATION: D3405".



SECTION B-B
CURB & GUTTER

LOOP DETECTOR INSTALLATION DETAIL



TYPICAL PLAN OF LOOP DETECTOR

LOOP DETECTOR PLACED IN CRUSHED AGGREGATE BASE (NEW ASPHALTIC PAVEMENT)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2014 — DATE
/S/ Ahmet Demirbilek — STATE ELECTRICAL
FHWA

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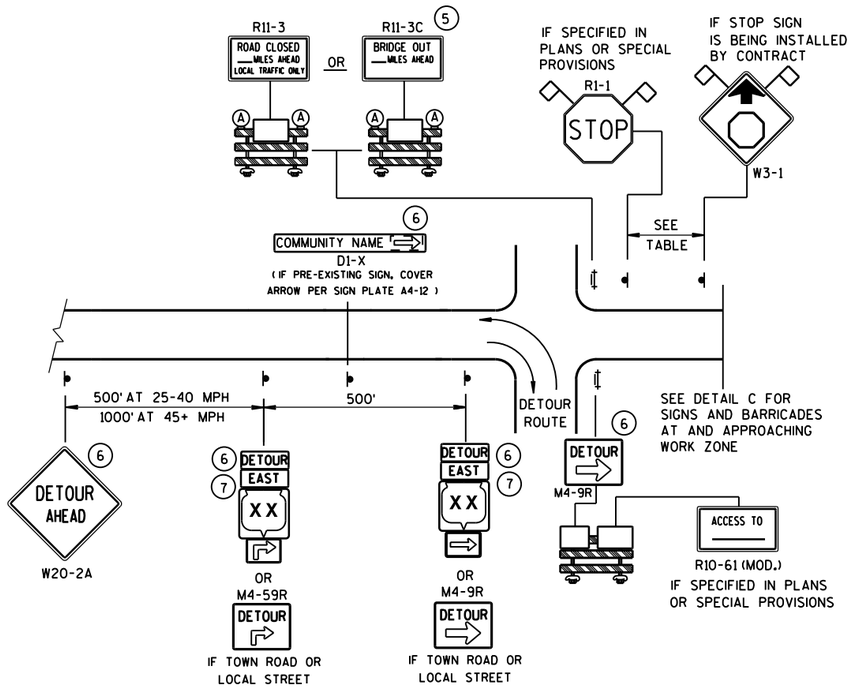
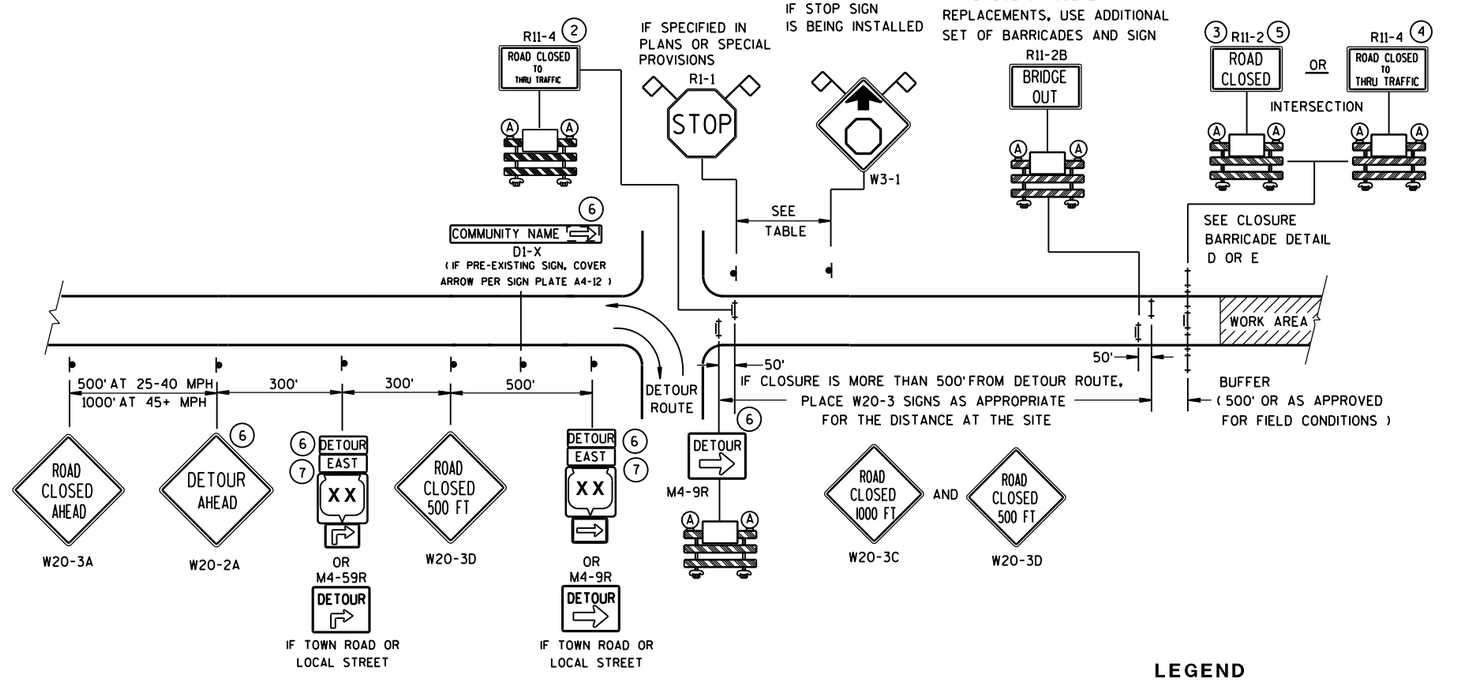


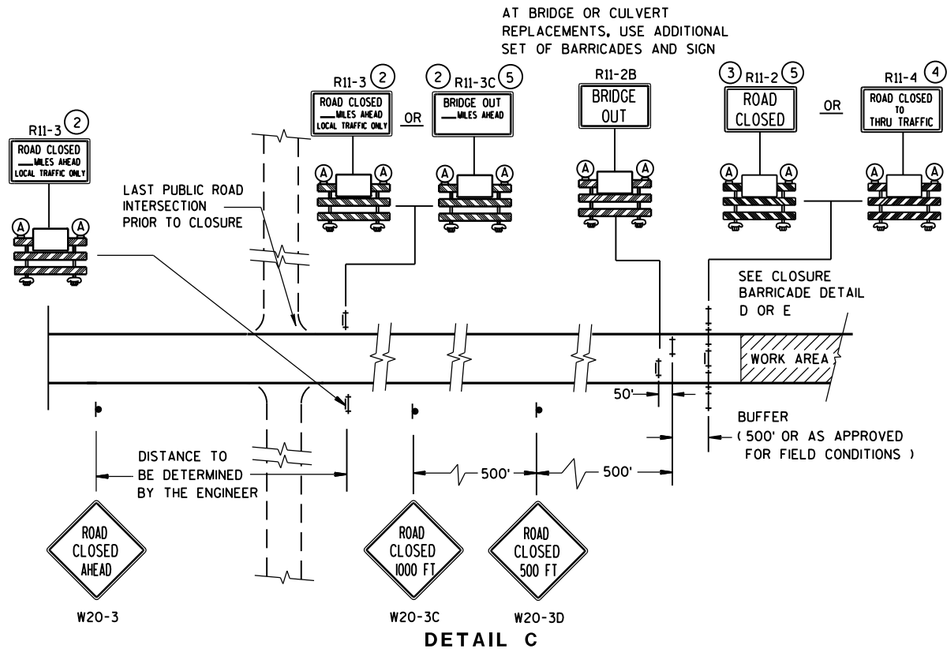
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

SEE SDD 15C2-SHEET "**b**" FOR GENERAL NOTES AND FOOTNOTES (1) THROUGH (7)

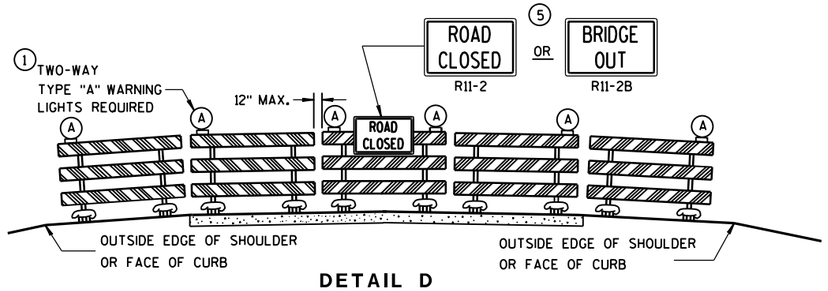
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA

S.D.D. 15 C 2-6a

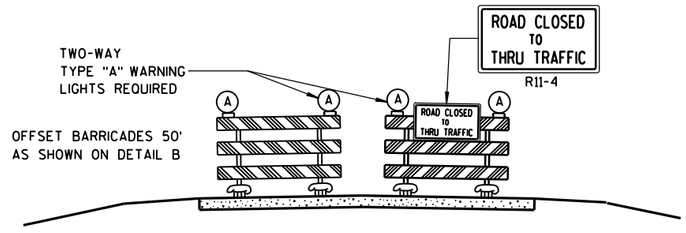
S.D.D. 15 C 2-6a



DETAIL D

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E

LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 — DATE

/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO SAFETY ENGI — FHWA

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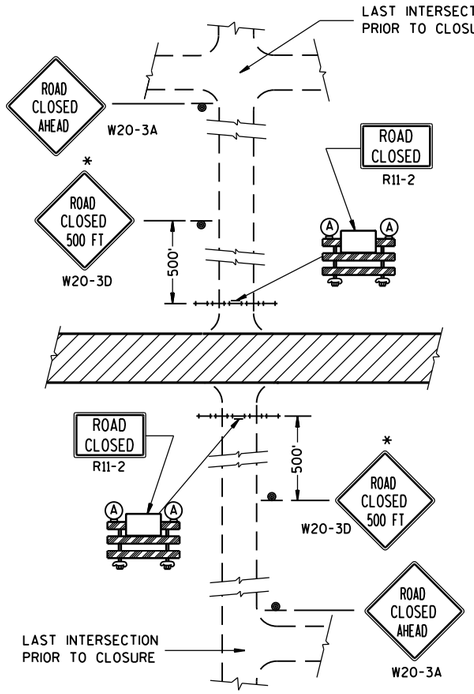
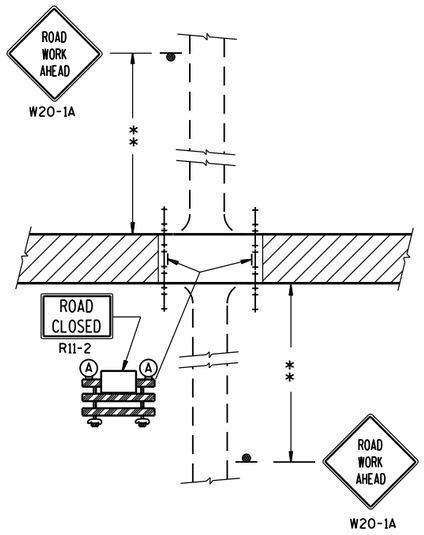
S.D.D. 15 C 2-6b



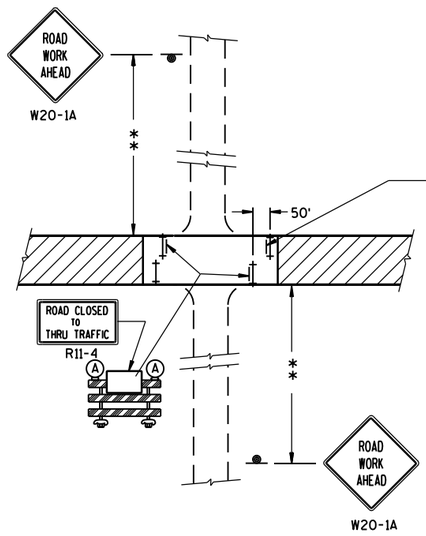
DETAIL 1

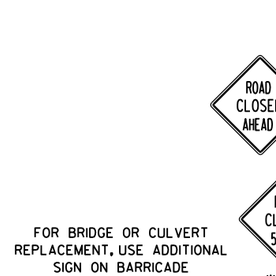
(NO ACCESS TO PROJECT)



DETAIL 2

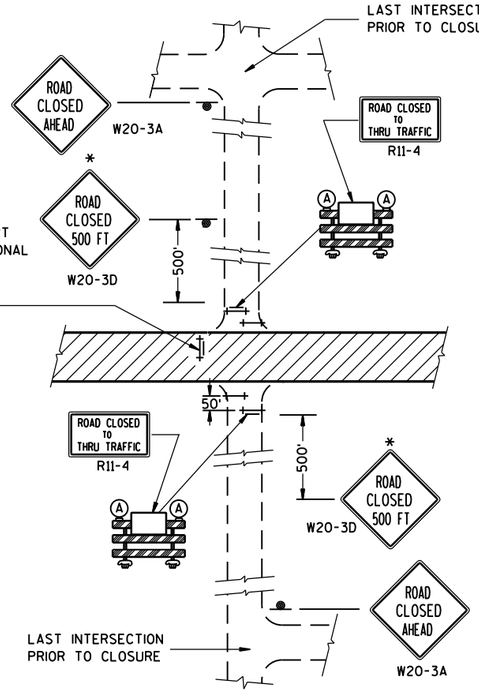
(PUBLIC CROSS-TRAFFIC MAINTAINED. NO ACCESS TO PROJECT).





DETAIL 3

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO[illegible]FIC SAFETY ENGI[illegible]
FHWA

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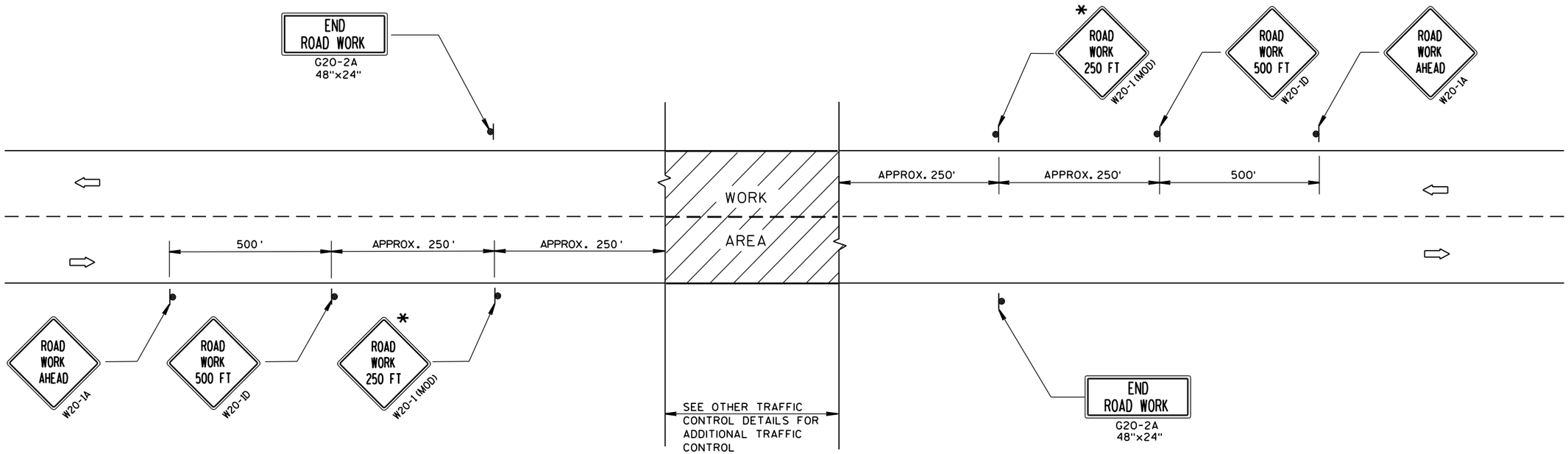
S.D.D. 15 C 3-3

TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL



GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

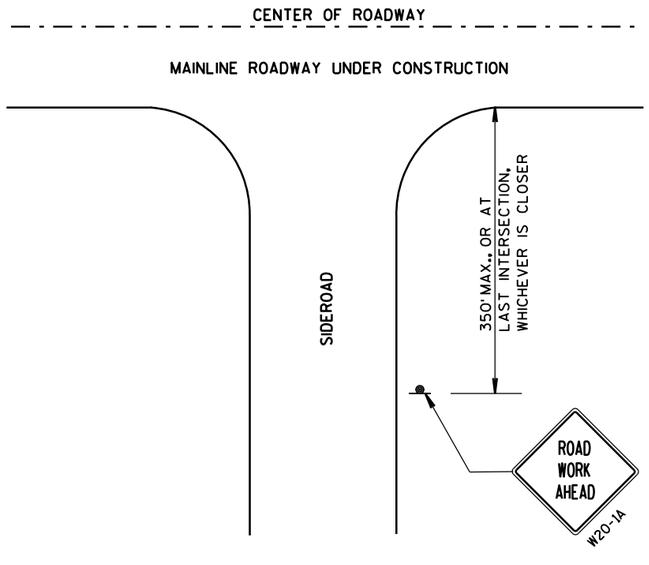
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE SAFETY ENGINEER
FHWA

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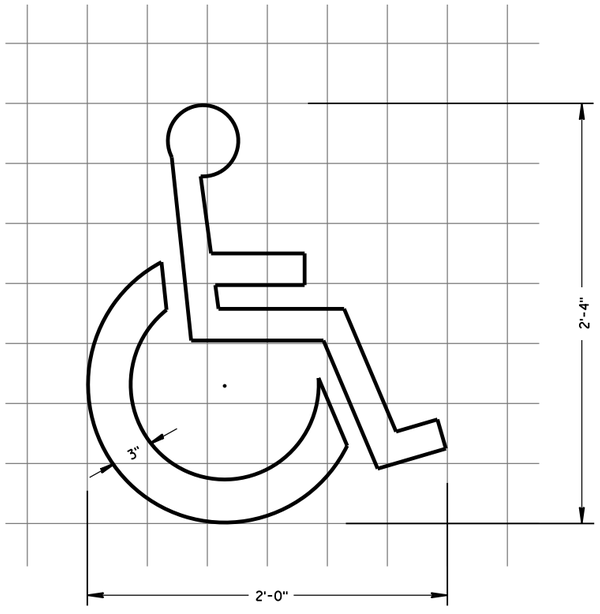
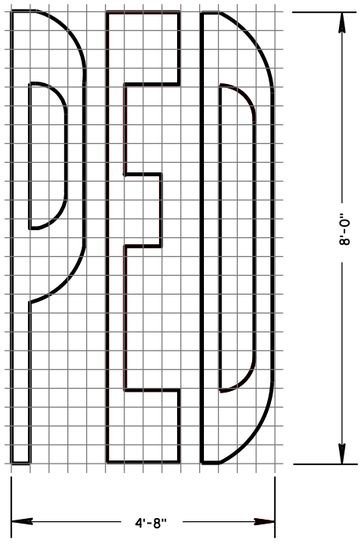
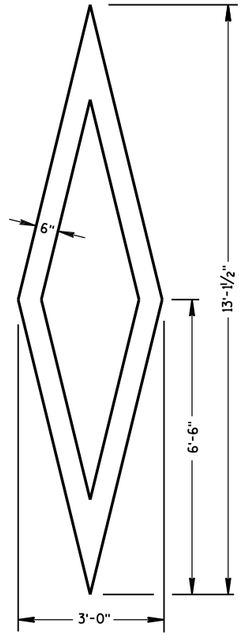
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



HANDICAP SYMBOL





PREFERENTIAL LANE SYMBOL

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S.D.D. 15 C 7-14a

PAVEMENT MARKING SYMBOLS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

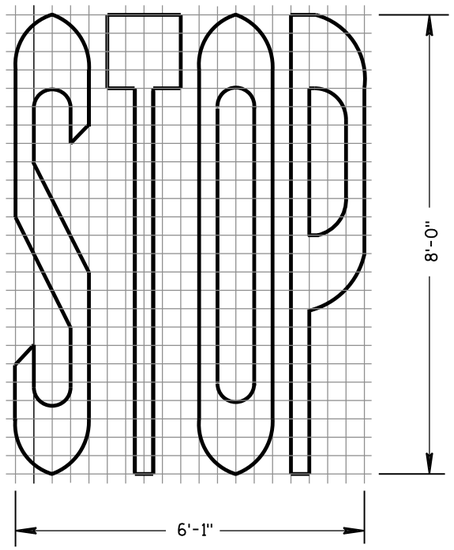
June 2017 — DATE

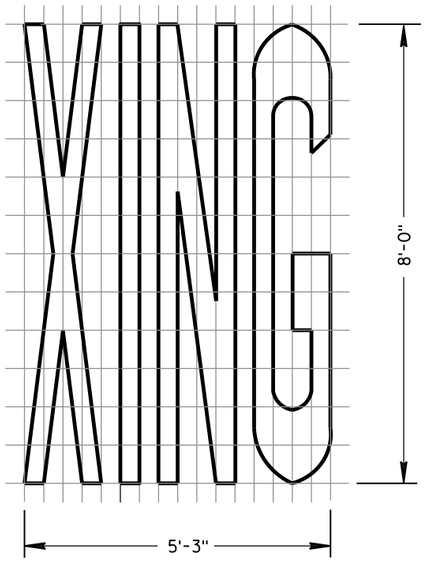
/S/ Matthew R. Rauch — STATE SIGNING AND MARK EER

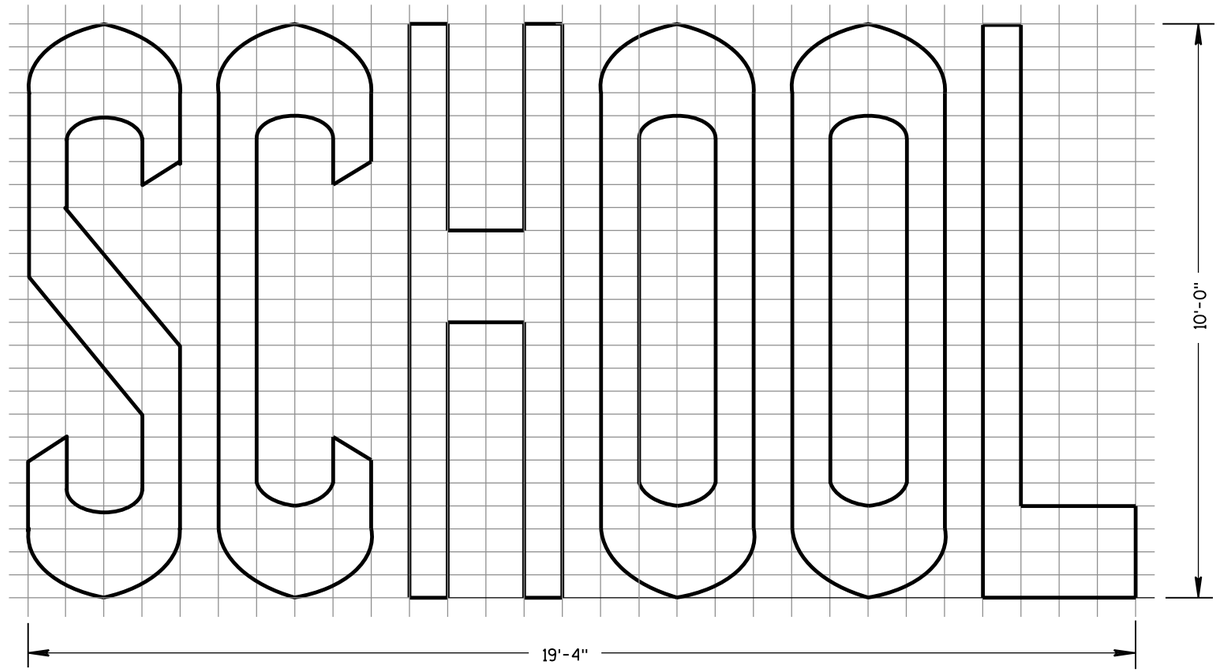
FHWA

GENERAL NOTES

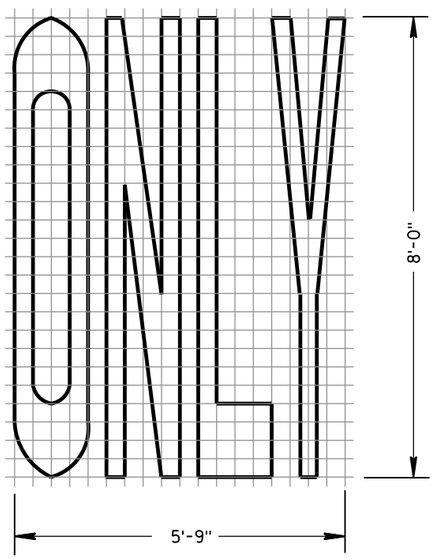
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

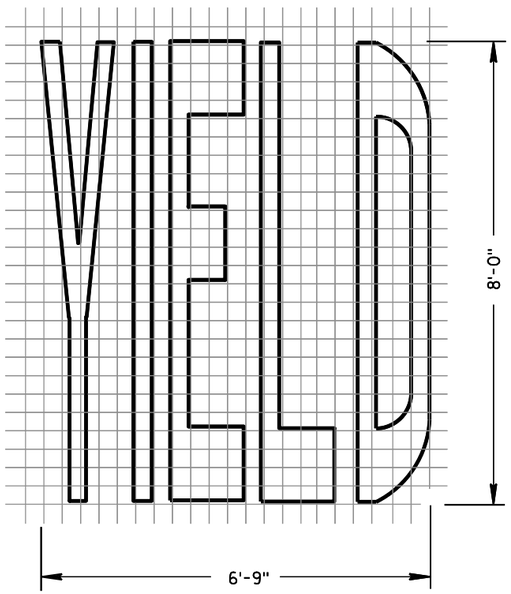


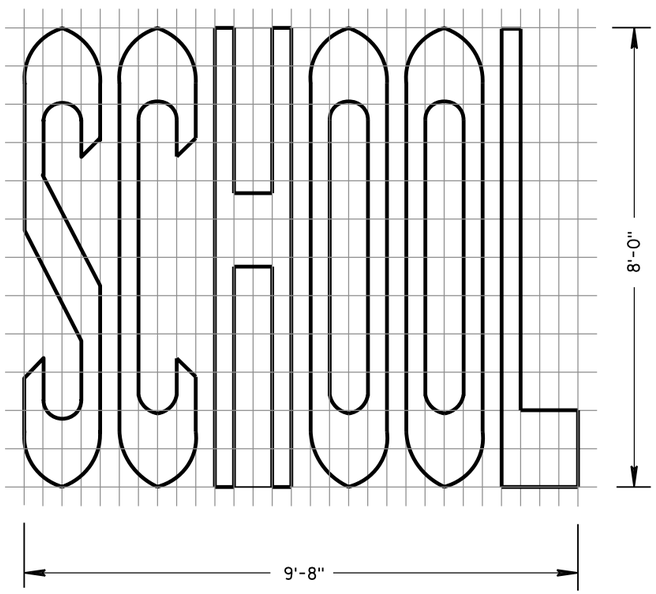




TWO-LANE







SINGLE-LANE

PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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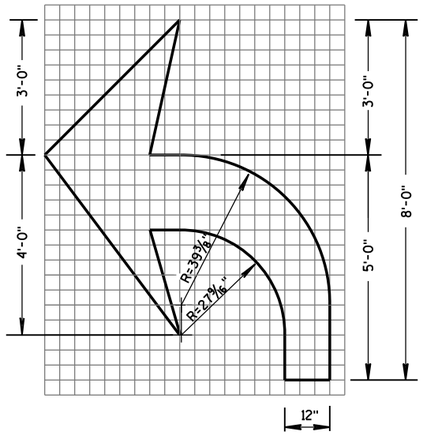
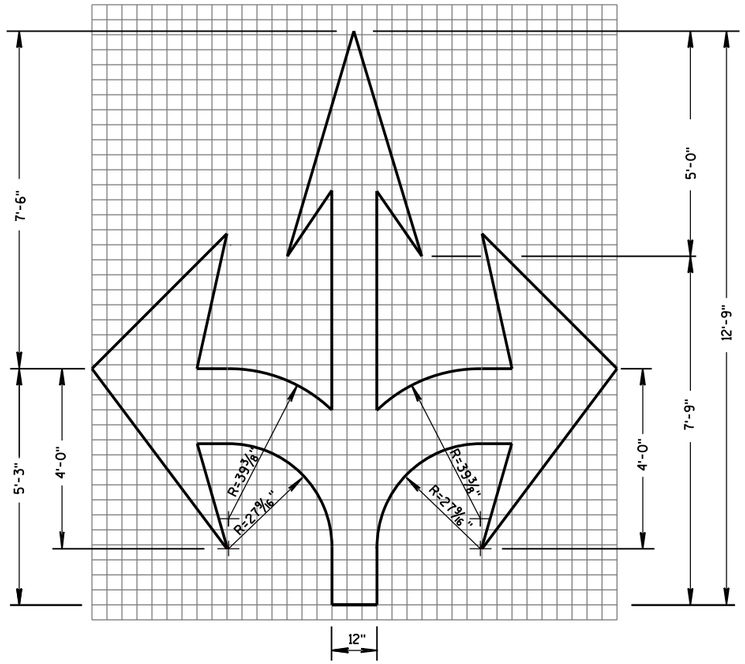
S.D.D. 15 C 7-14b

GENERAL NOTES

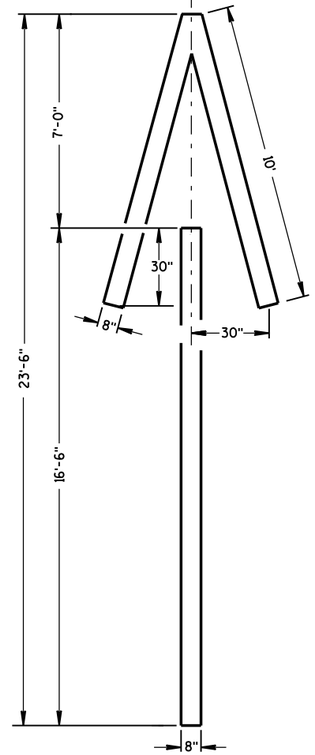
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



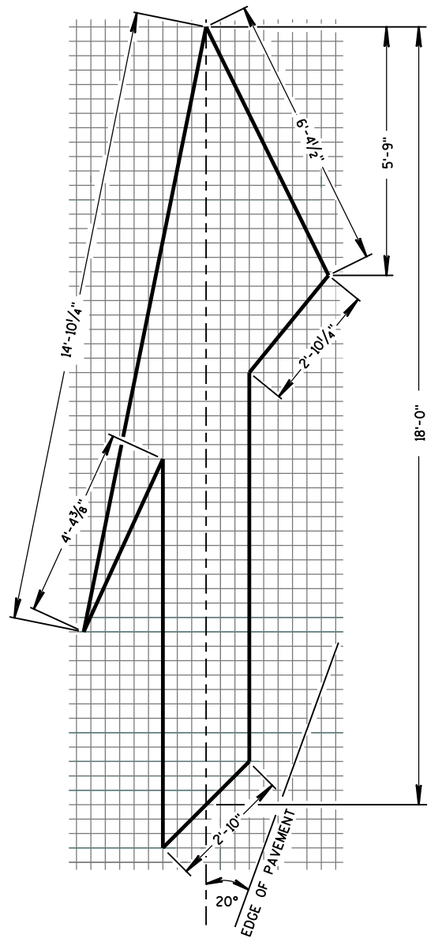
TYPE 2



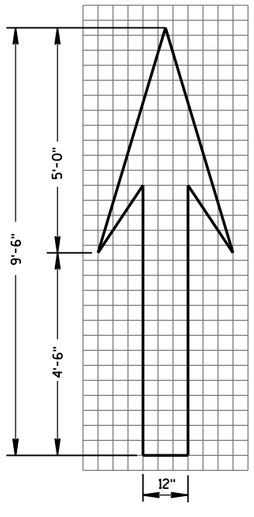
TYPE 6



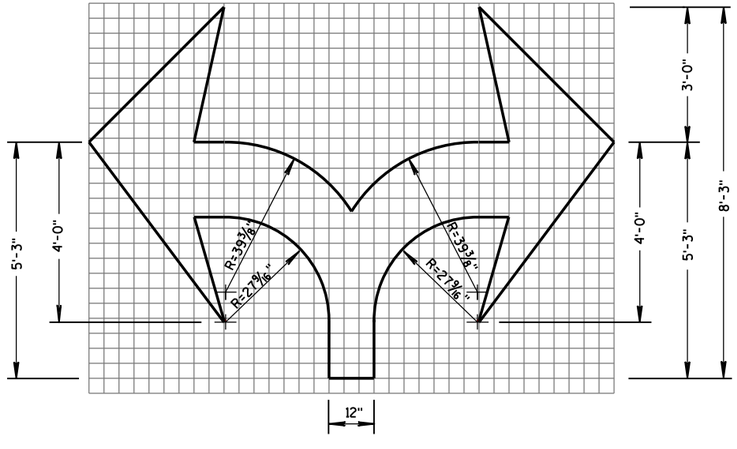
TYPE 4



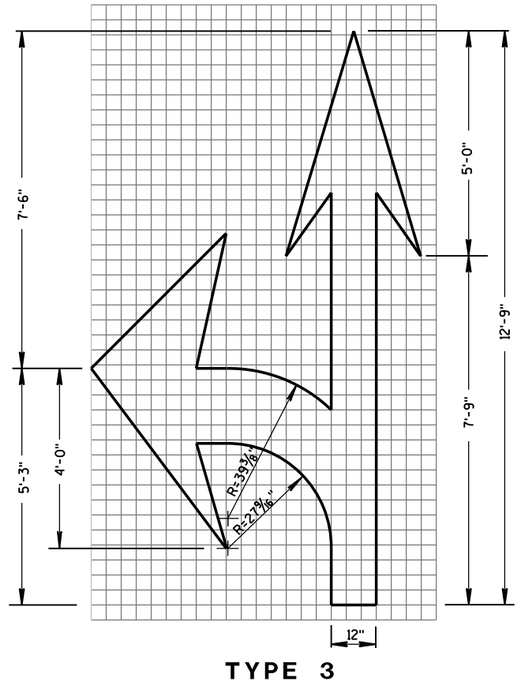
TYPE 5 LANE DROP ARROW



TYPE 1



TYPE 7



TYPE 3

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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June 2017 — DATE
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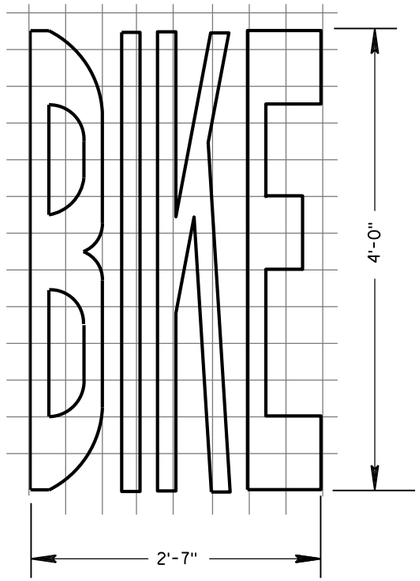
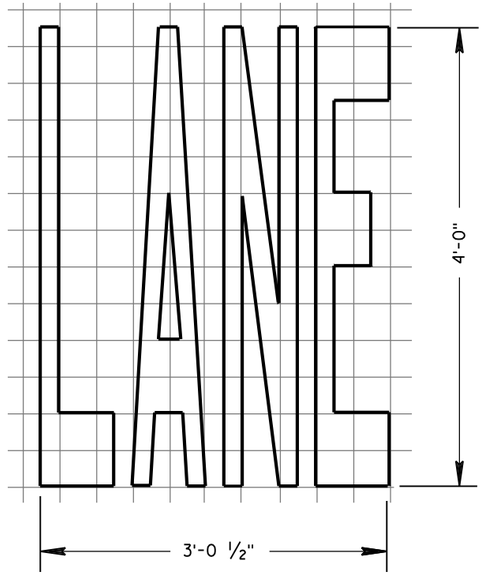
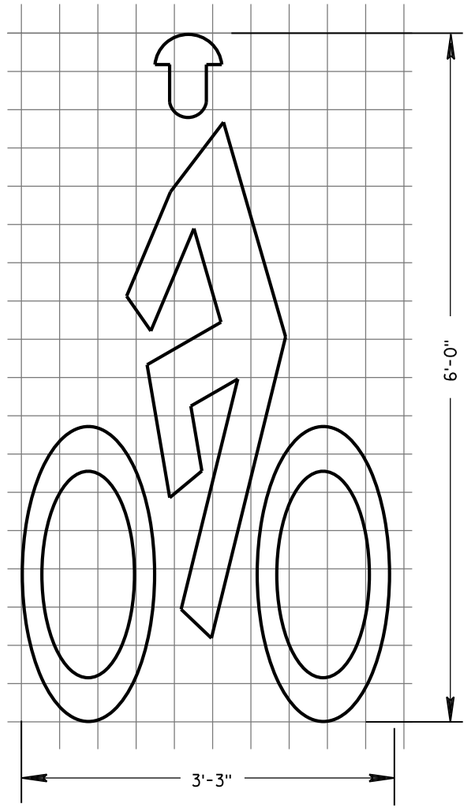
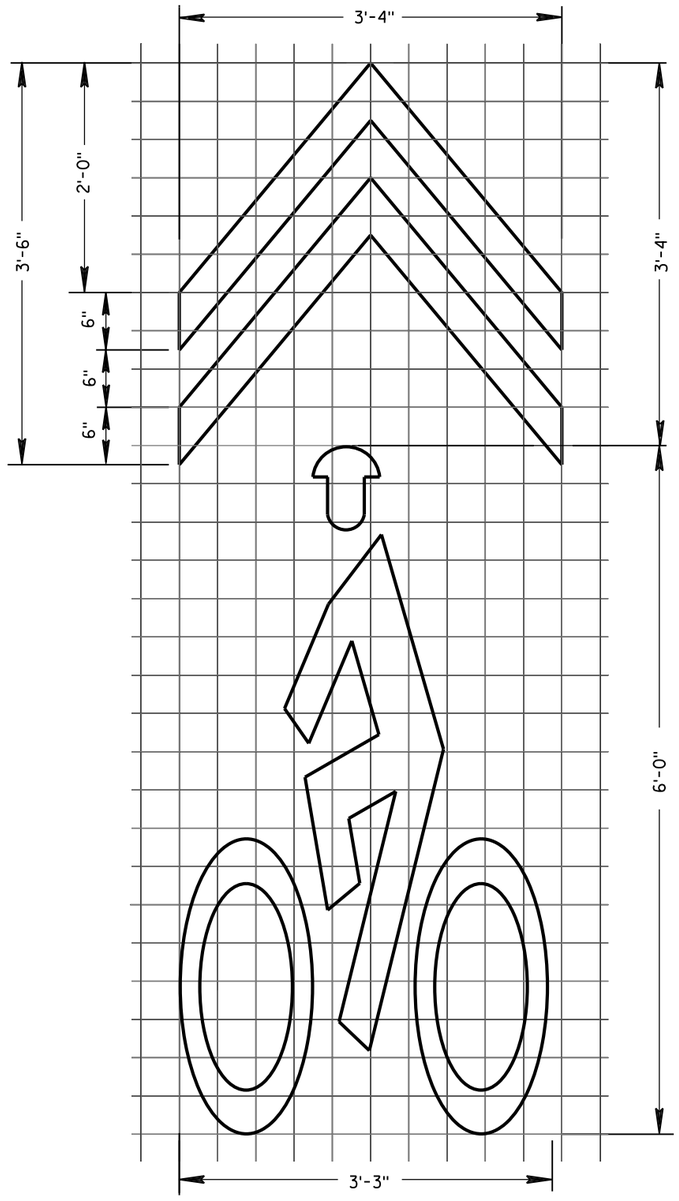




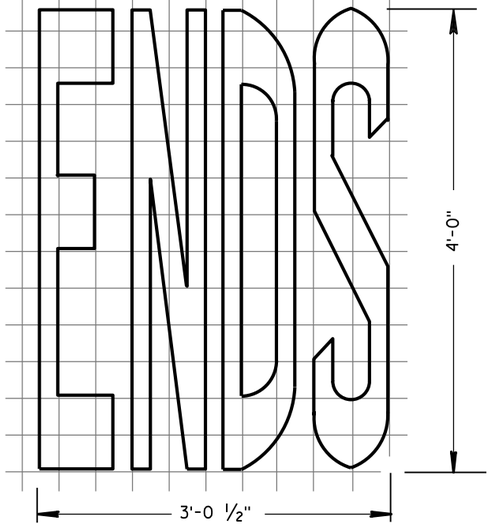
BIKE LANE WORDS



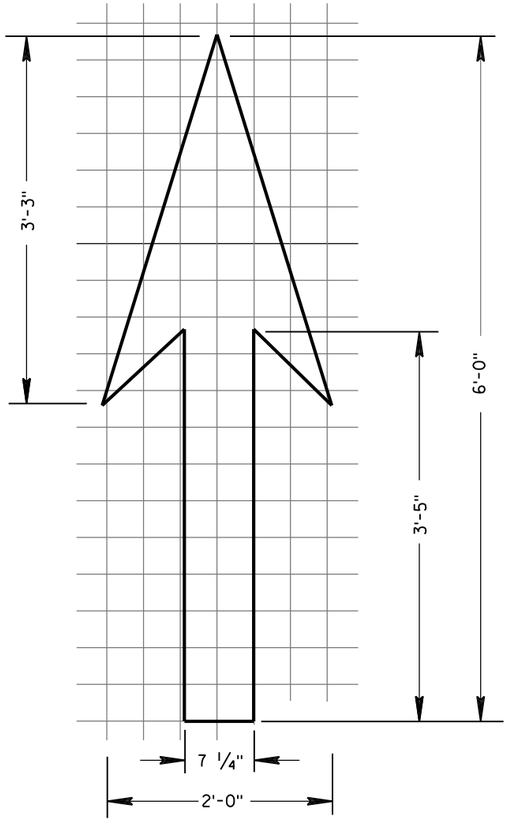
BIKE LANE SYMBOL



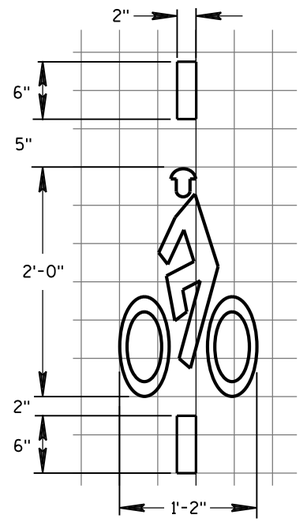
BIKE SYMBOL FOR SHARED LANE



BIKE LANE WORDS



BIKE LANE ARROW



BICYCLE DETECTOR PAVEMENT MARKING

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING FOR BIKE LANES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MARKING ENGINEER
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S.D.D. 15 C 7-14e

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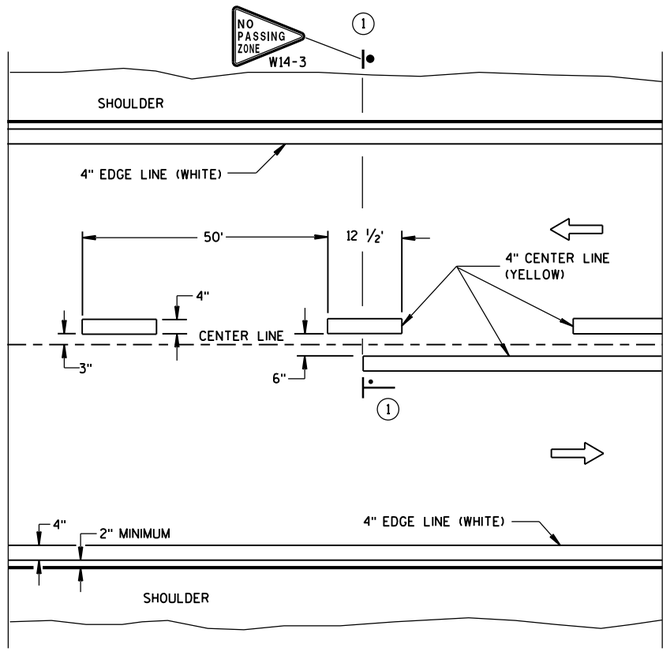
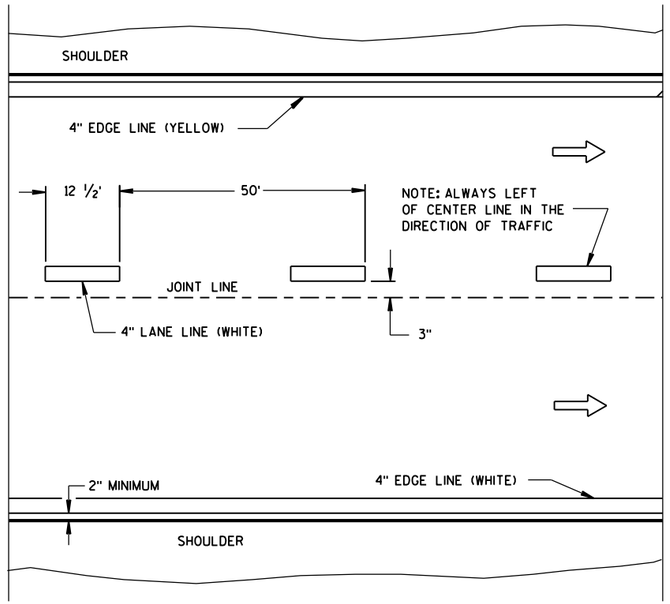
S.D.D. 15 C 7-14e



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

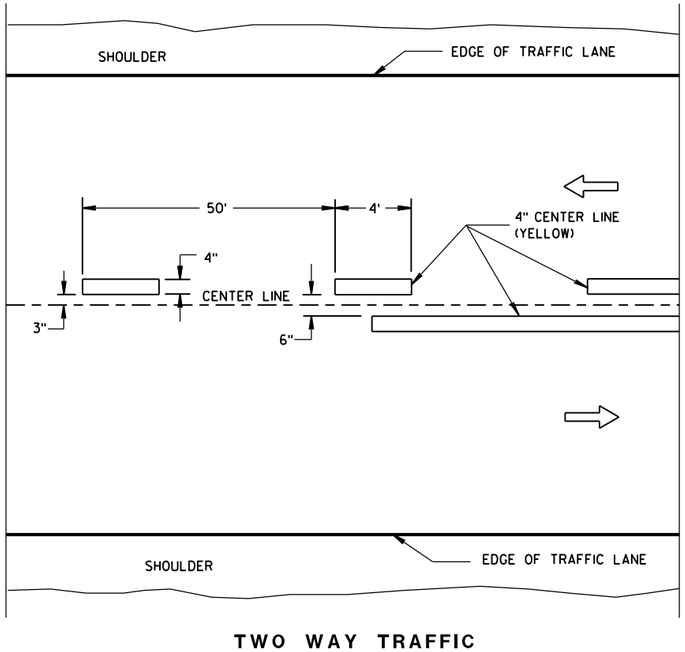
NOTE

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

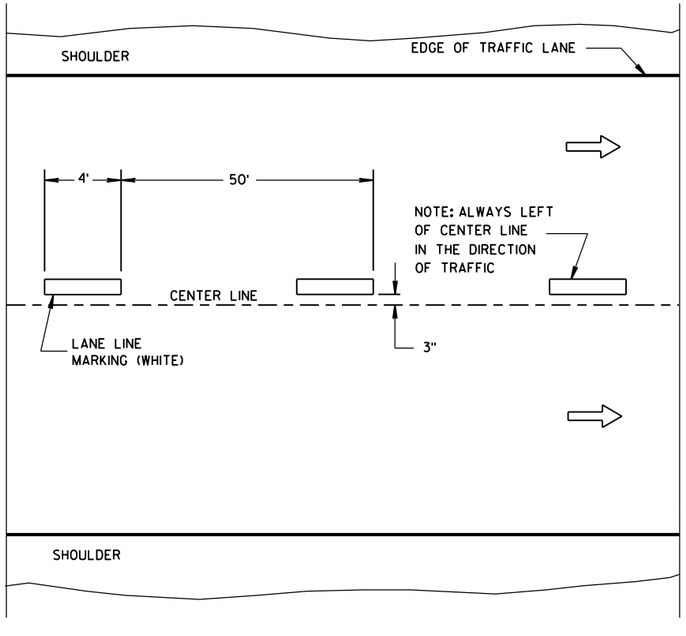
LEGEND

"T" MARKING

POST MOUNTED SIGN



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MAR[illegible]INEER
FHWA

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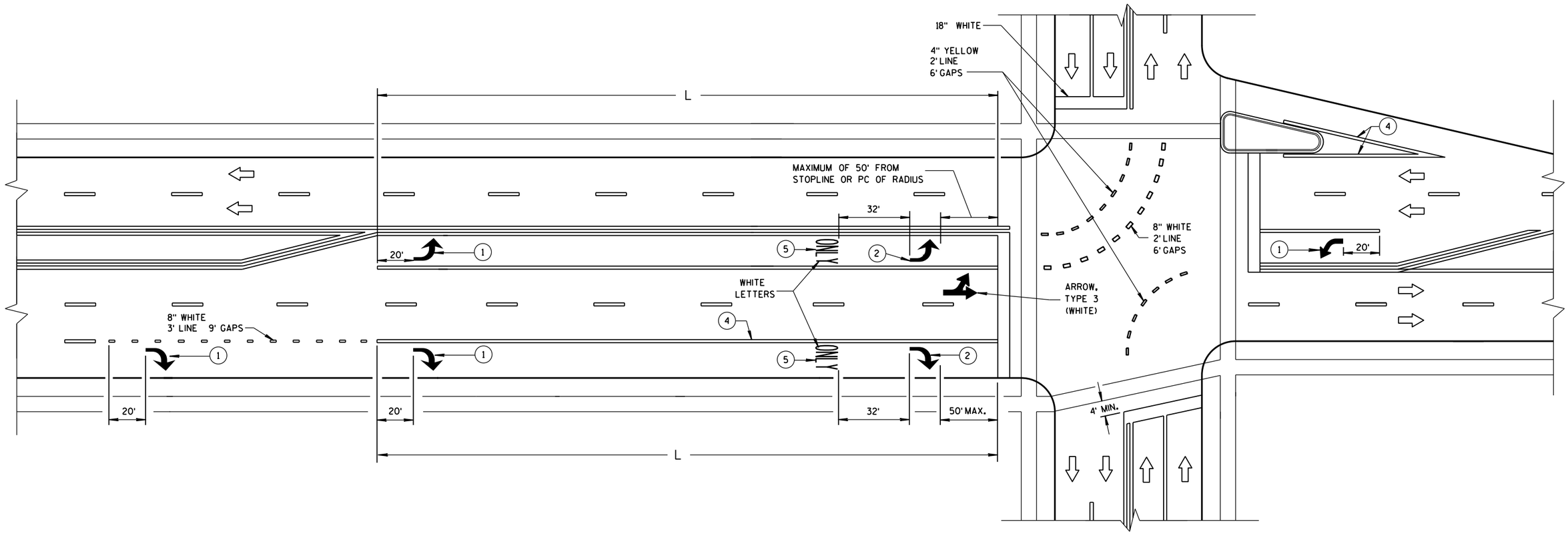
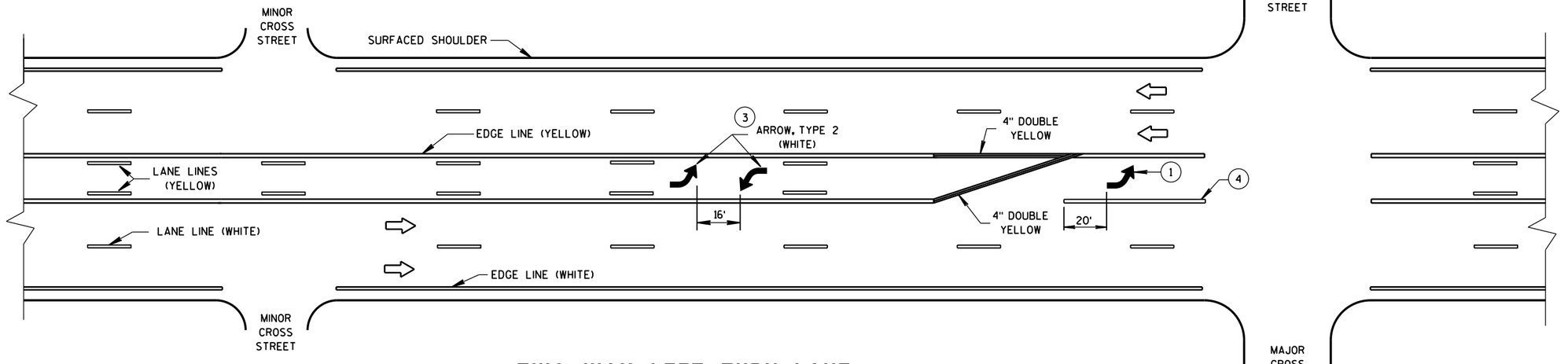
S.D.D. 15 C 8-18a





GENERAL NOTES

(1) REQUIRED ARROW, TYPE 2 (WHITE).

(2) REQUIRED ARROW, TYPE 2 (WHITE) WHEN L IS GREATER THAN 78 FEET AND LESS THAN OR EQUAL TO 166 FEET.

(3) A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.

(4) 8" WHITE

(5) REQUIRED WORD ONLY WHEN L IS GREATER THAN 166 FEET.

NOTE:
ARROW SYMBOL (⇨)
SHOWS DIRECTION OF TRAVEL

L = LENGTH OF TURN BAY

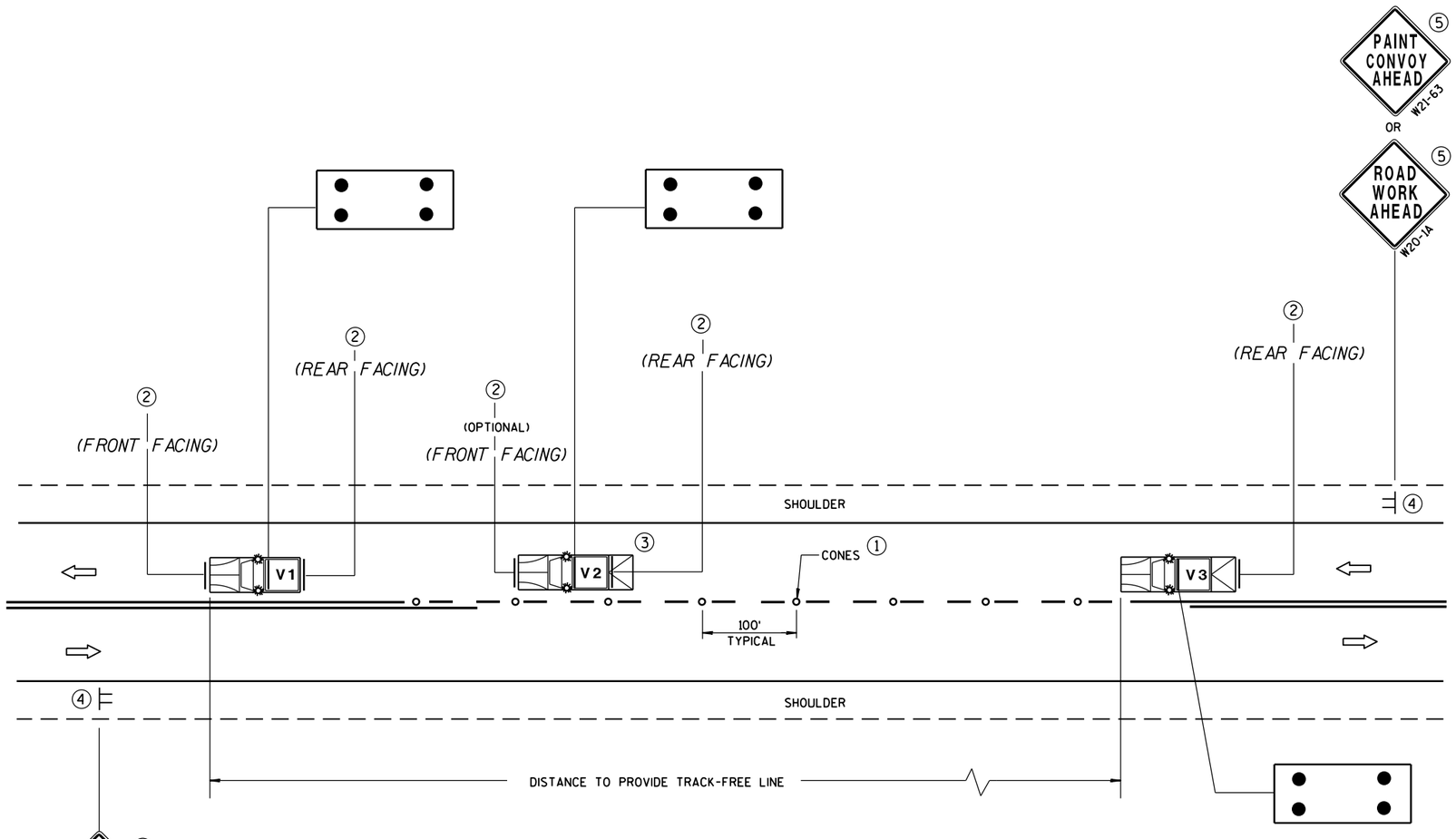
TWO WAY LEFT TURN LANE

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 15 C 8-18b



MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

(1) CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

(2) USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.

WET PAINT W21-64 OR WET PAINT W21-64

(3) OPTIONAL TRUCK-MOUNTED ATTENUATOR.

(4) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

(5) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

- **V1** LEAD VEHICLE
- **V2** SHADOW VEHICLE
- **V3** TRAIL VEHICLE WITH TMA
- **TMA** TRUCK-MOUNTED ATTENUATOR
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- CONES
- FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2016 DATE FHWA	/S/ Peter Amakobe Atepe STATEWIDE WORK ZON SAFETY ENGINE

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S.D.D. 15 C 19-4a

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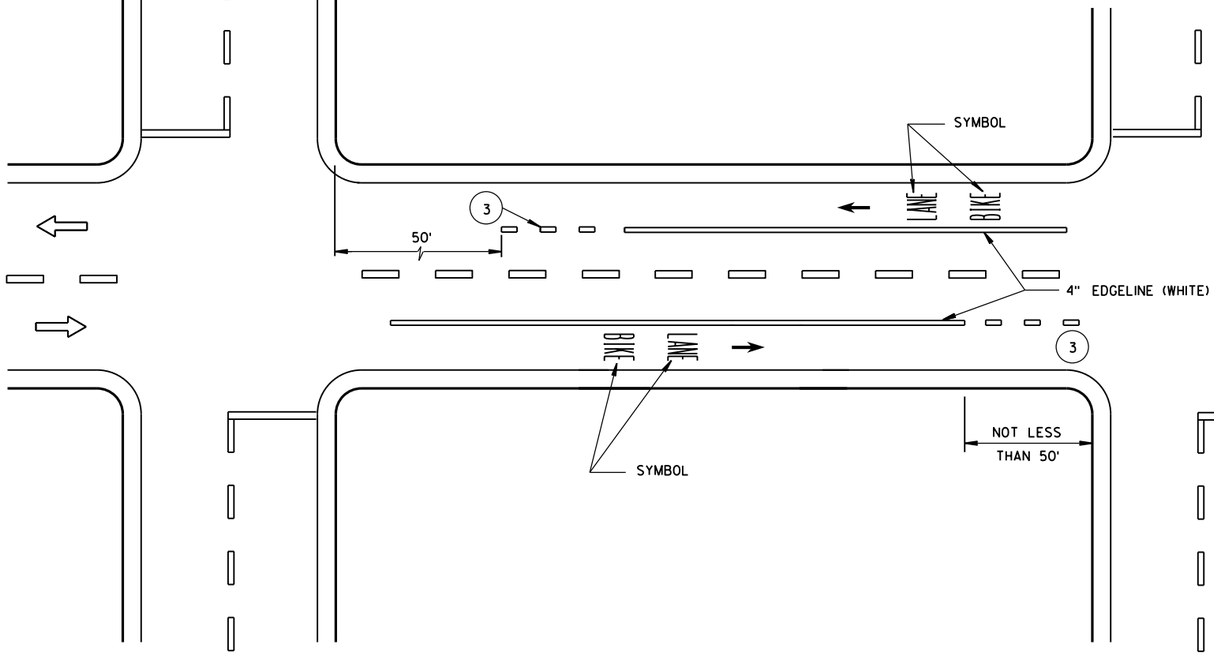
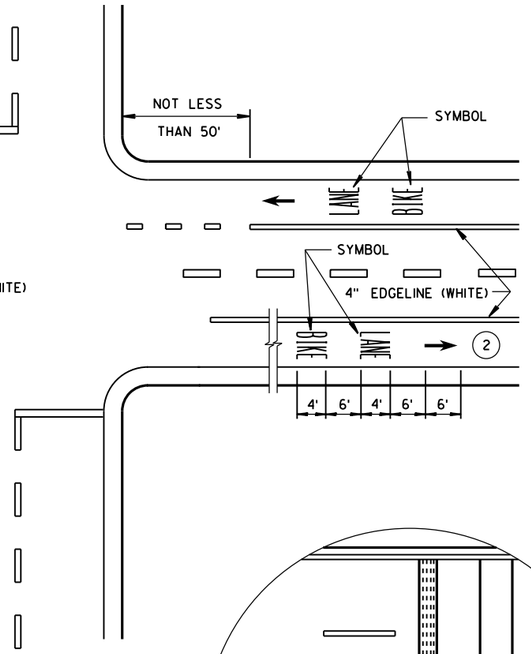
S.D.D. 15 C 19-4a



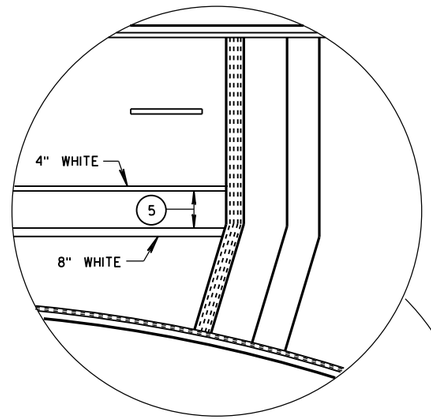


DESIGNATED BICYCLE LANE
NO PARKING

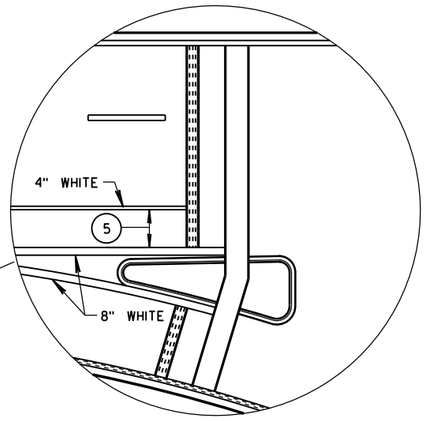
GENERAL NOTES

1. DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
2. MINIMUM OF ONE PER BLOCK. MAXIMUM OF 250 FEET.
3. DOTTED LINES (3'LINE, 9'GAP) SHOULD BE USED 50 FEET TO 200 FEET IN ADVANCE OF AN INTERSECTION WHERE THERE IS NO RIGHT TURN ONLY LANE AND THERE IS HEAVY RIGHT TURN TRAFFIC OR THERE IS A NEAR-SIDE BUS STOP. AT OTHER INTERSECTIONS WHERE RIGHT TURN TRAFFIC IS LIGHT TO MODERATE, A SOLID LINE CAN BE USED UP TO THE INTERSECTION.
4. IF SIGNED AND/OR MARKED AS A BICYCLE FACILITY INCLUDE SECOND LINE OF LINE-SPACE MARKING, OTHERWISE DO NOT.
5. BIKE ACCOMODATION FOR CONCRETE PAVEMENT IS 5 FEET WIDE. BIKE ACCOMODATION FOR ASPHALT PAVEMENT IS A MINIMUM OF 4 FEET. USE 5 FEET AT ≥45 MPH.
6. OMIT THESE MARKINGS FOR WIDER TURN LANE APPLICATIONS (MINIMUM OF 15 FEET WIDE TURN LANE).
7. REFER TO CONTRACT PLANS FOR LANE WIDTH.

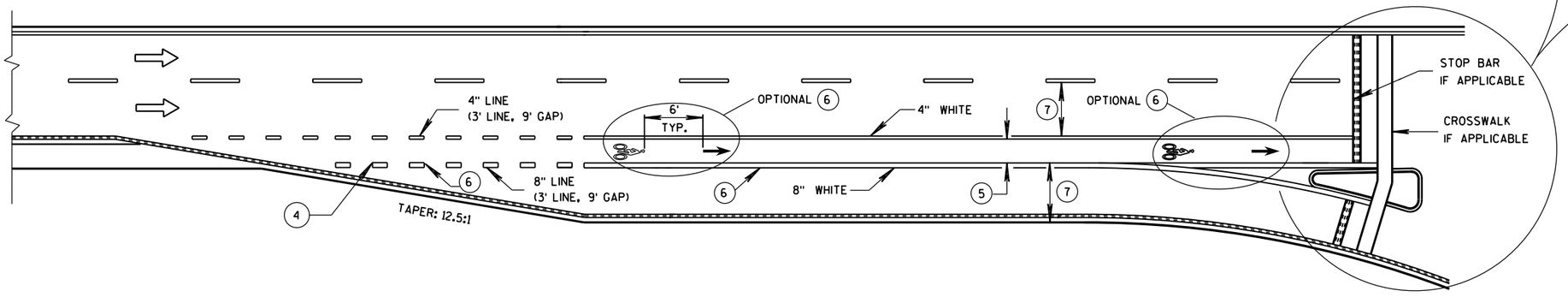
⇨ DIRECTION OF TRAVEL



4 LANE DIVIDED WITHOUT ISLAND



4 LANE DIVIDED WITH ISLAND



BIKE LANE - FOR 2-LANE ROADWAYS AND 4-LANE DIVIDED ROADWAYS
(4-LANE DIVIDED WITH RIGHT TURN LANE SHOWN)

BICYCLE LANE MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
12-2016 DATE
/S/ Matthew R. Rauch
STATE SIGNING AND MAF NEER
FHWA

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S.D.D. 15 C 29-5a

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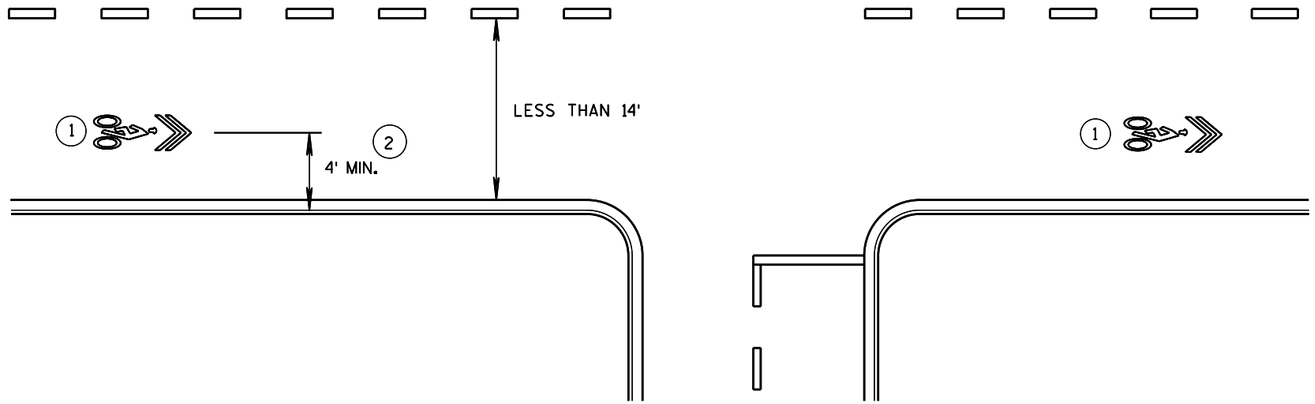
S.D.D. 15 C 29-5a

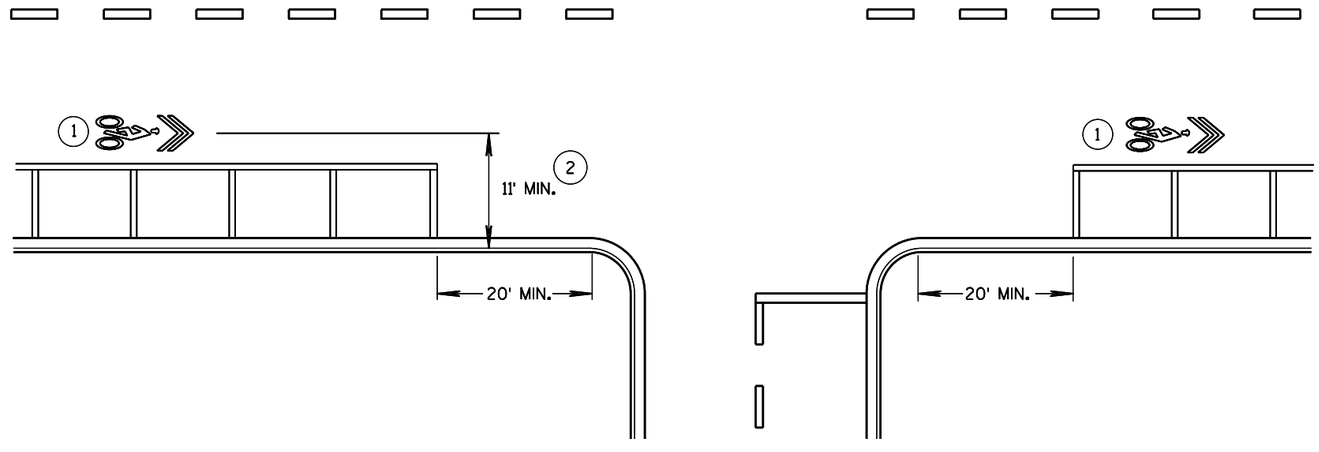
GENERAL NOTES

(1) MINIMUM OF ONE PER BLOCK, MAXIMUM OF 250 FEET.

(2) OR TO EDGE OF PAVEMENT WITHOUT CURB.



WITHOUT PARKING



WITH PARKING

PAVEMENT MARKING FOR
SHARED LANE
35 MPH OR LESS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
12-2016 DATE
/S/ Matthew R. Rauch
STATE SIGNING AND MARK EER
FHWA

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S.D.D. 15 C 29-5b

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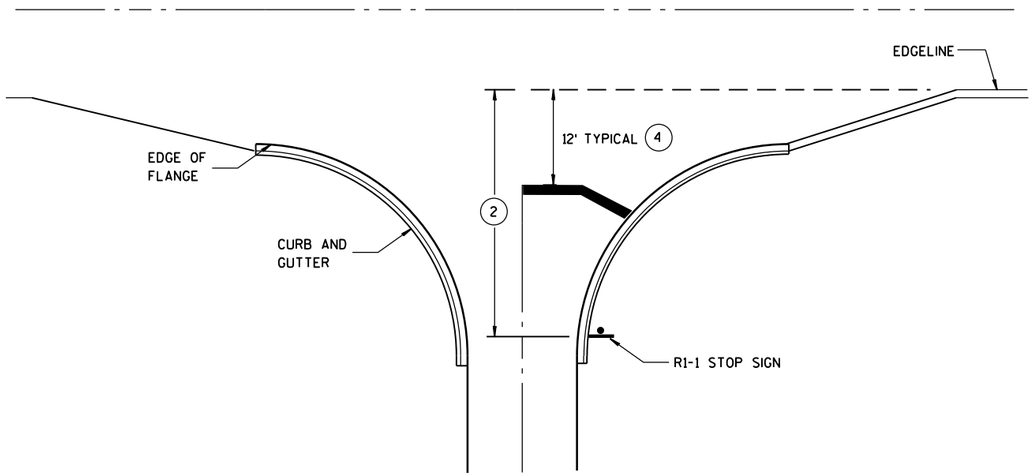
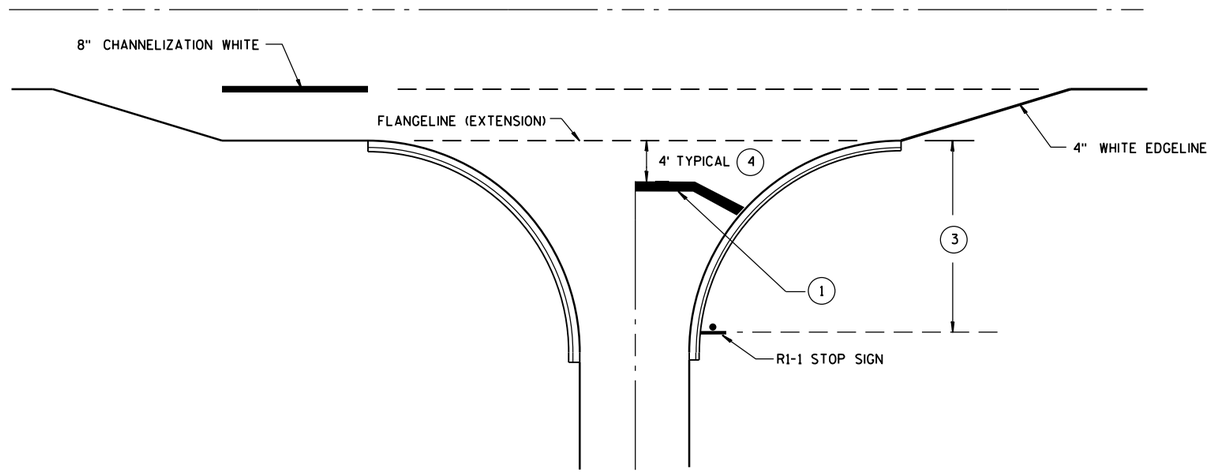
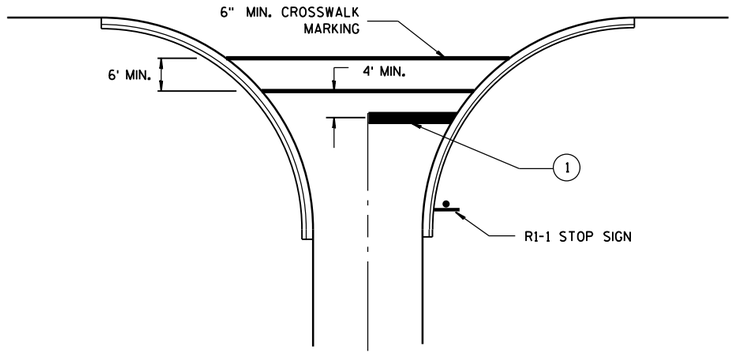
S.D.D. 15 C 29-5b



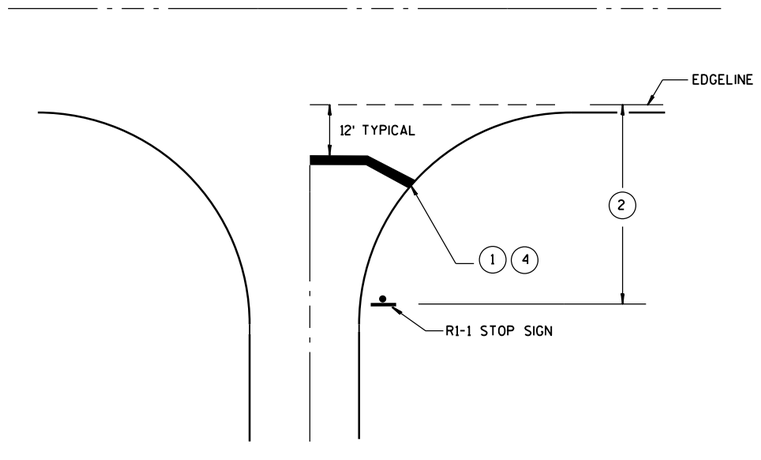
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
3. IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
4. MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

4-18-2016
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MAR[illegible]NEER

FHWA

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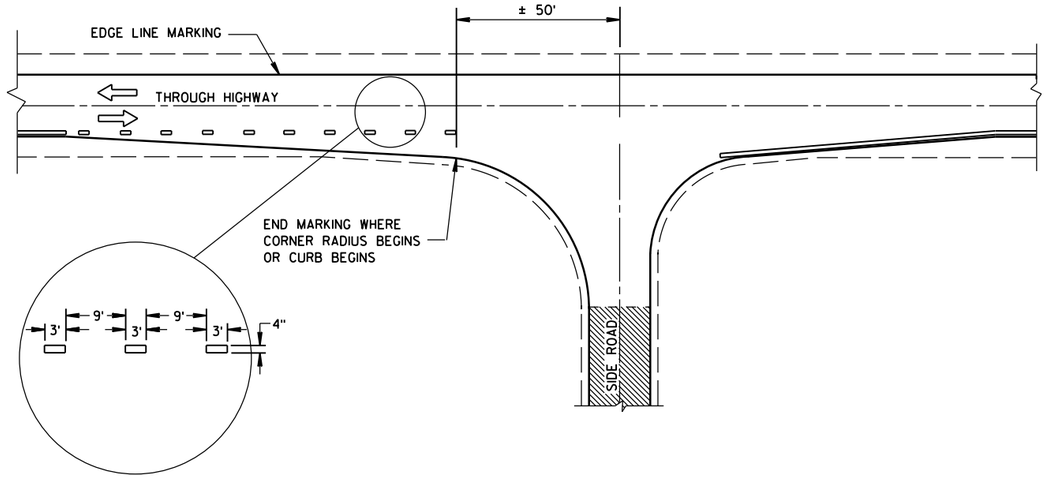
S.D.D. 15 C 33-2



MINOR INTERSECTION WITHOUT CURBS

GENERAL NOTES

EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.

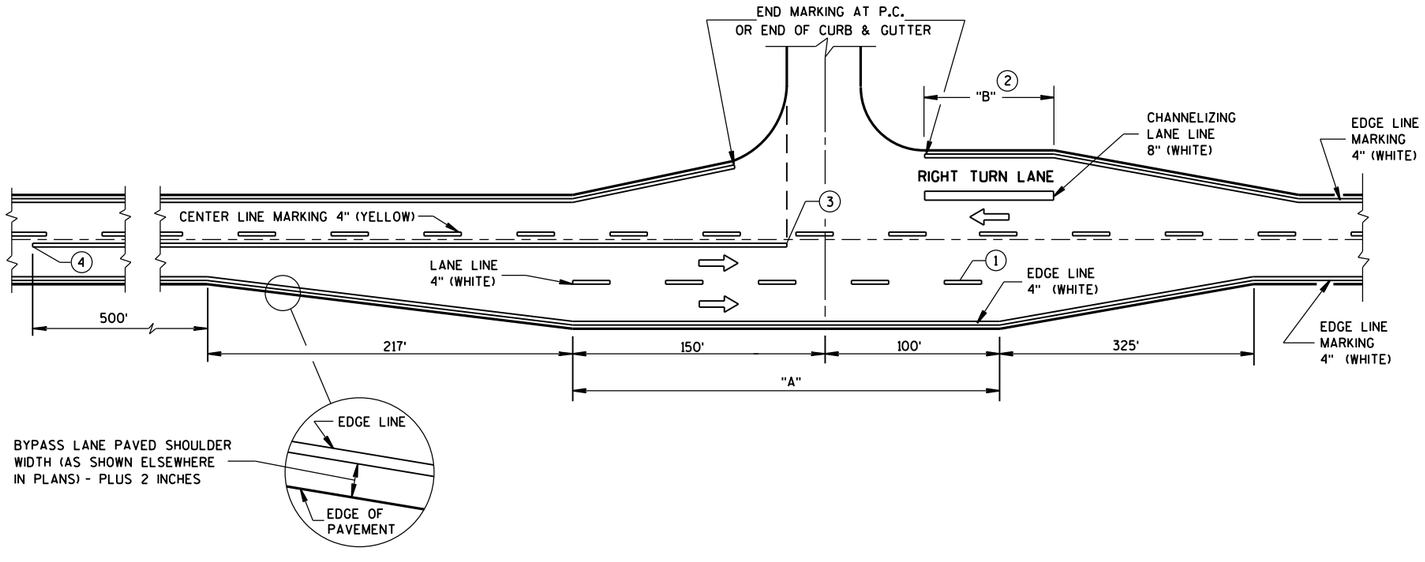
(1) WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.

(2) WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.

(3) BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.

(4) BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL



MAJOR INTERSECTIONS

(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)

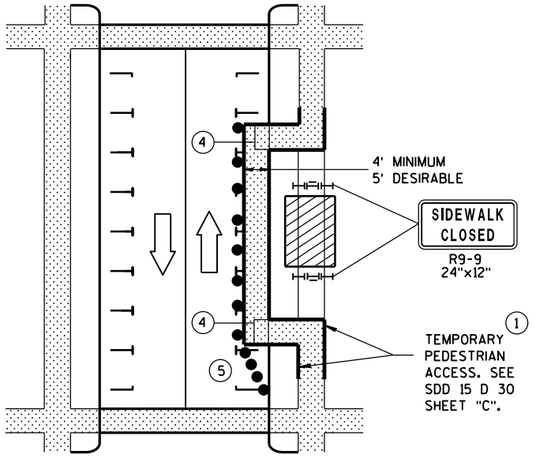
PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

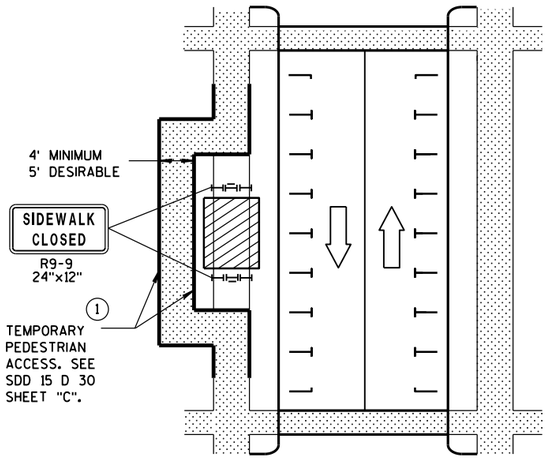
S.D.D. 15 C 35-1a

NOTE: MAY BE USED ON ROADWAY WITH POSTED SPEED OF LESS THAN 40 MPH.

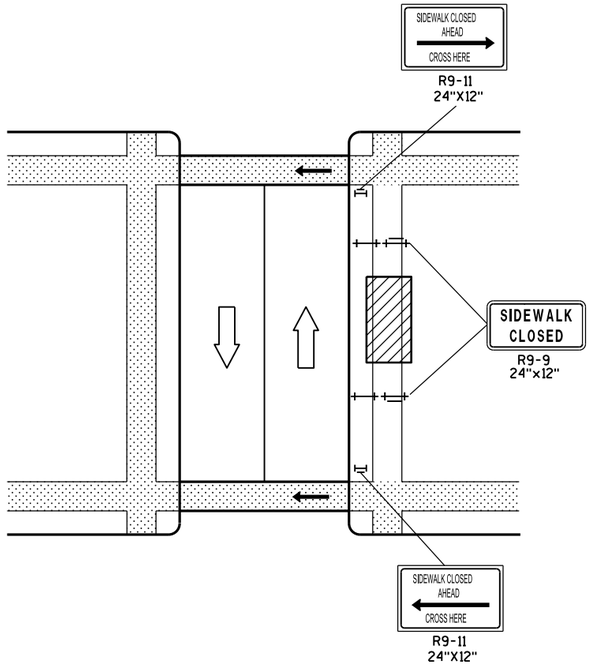


MID-BLOCK SIDEWALK CLOSURE IN PARKING LANE

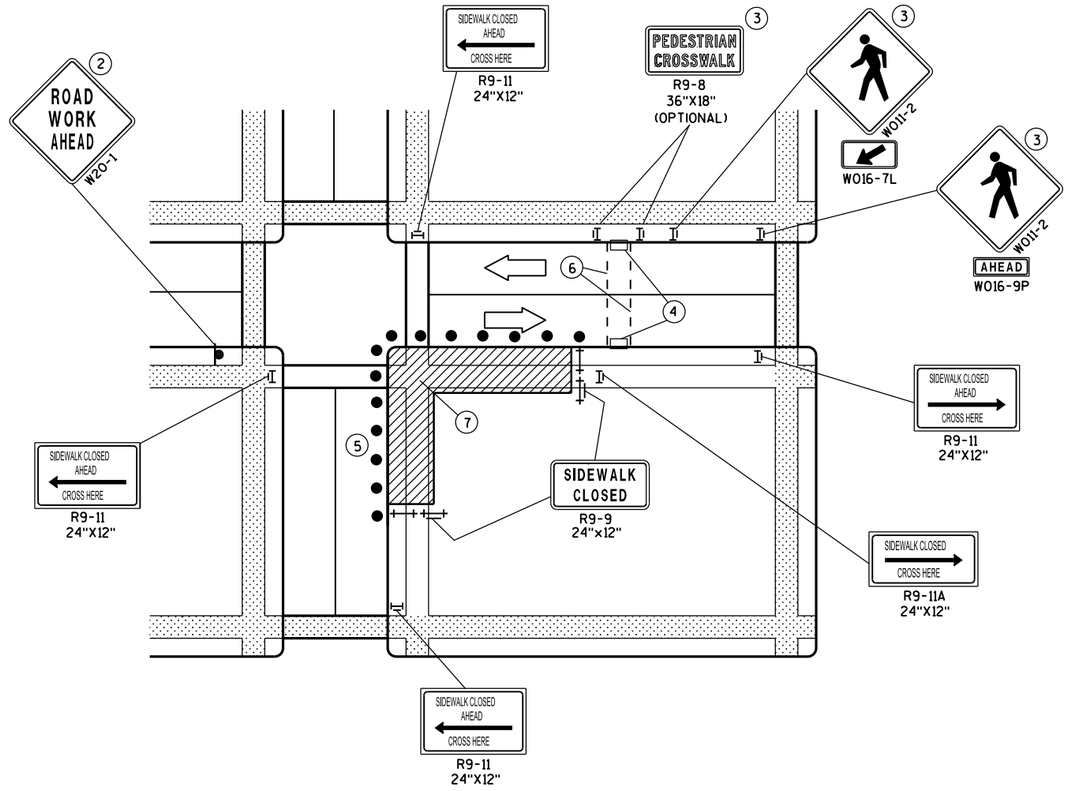
NOTE: LAYOUT SAME AS ABOVE.



SIDEWALK DIVERSION



MID-BLOCK SIDEWALK CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

(1) IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.

(2) "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.

(3) IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND W011-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.

(4) TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".

(5) DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.

(6) TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.

(7) LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

LEGEND

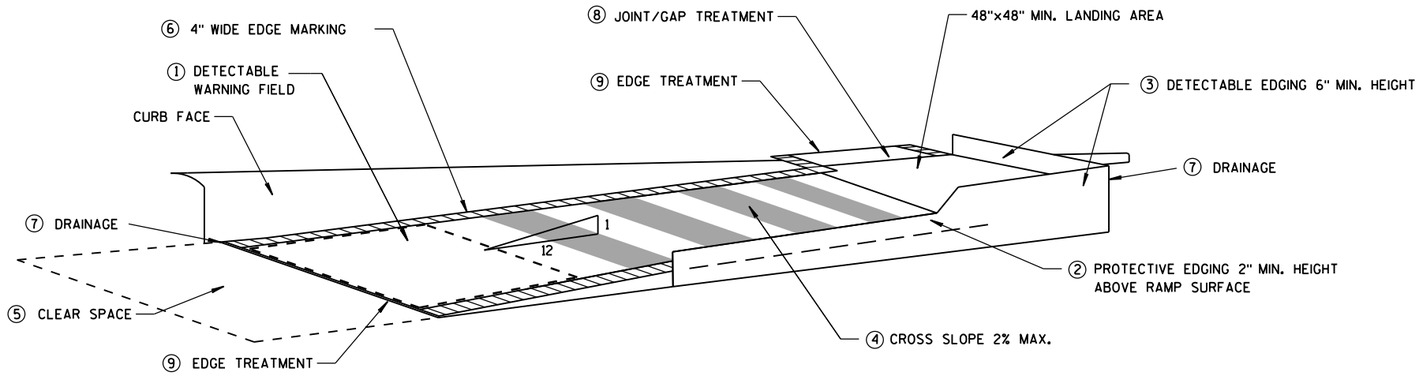
- SIGN ON PERMANENT SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- PEDESTRIAN CHANNELIZATION DEVICE
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)
- DIRECTION OF TRAFFIC
- TRAFFIC CONTOL DRUM

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 15 D 30-3a



TEMPORARY CURB RAMP PARALLEL TO CURB

GENERAL NOTES

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

① CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 8D5 SHEET "E".

② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

④ CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

⑤ CLEAR SPACE OF 48"X48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

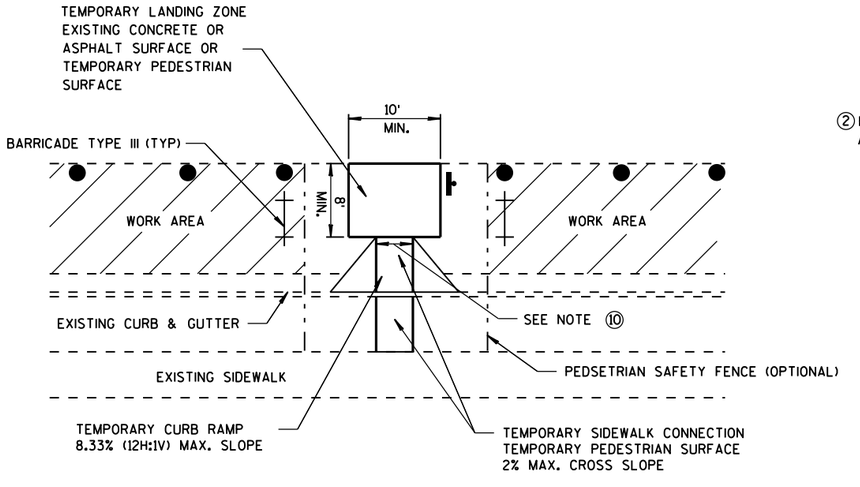
⑥ THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.

⑦ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

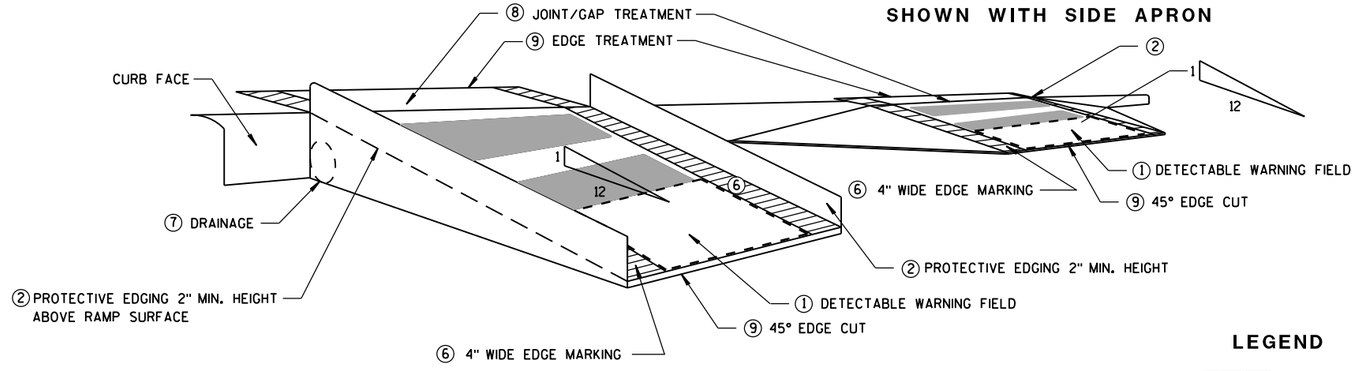
⑧ LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

⑨ CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

⑩ 5' WIDE MIN. WITH PEDSETRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.



TEMPORARY BUS STOP PAD



SHOWN WITH PROTECTIVE EDGE

SHOWN WITH SIDE APRON

TEMPORARY CURB RAMP PERPENDICULAR TO CURB

LEGEND

WORK AREA

TYPE III BARRICADE

TRAFFIC CONTOL DRUM

TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2016
DATE

/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO SAFETY ENGI IC

FHWA

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S.D.D. 15 D 30-3b

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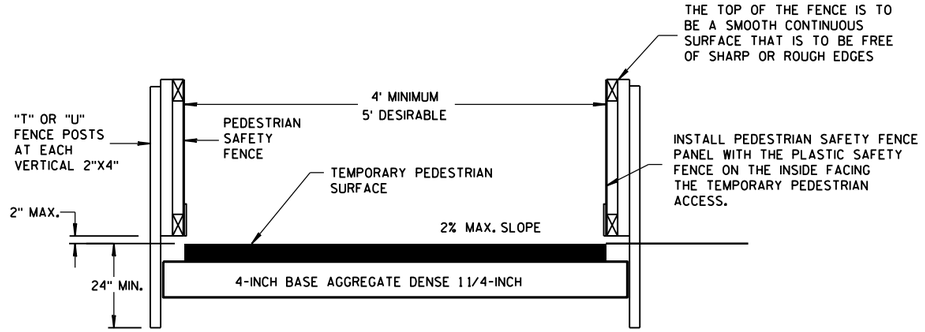
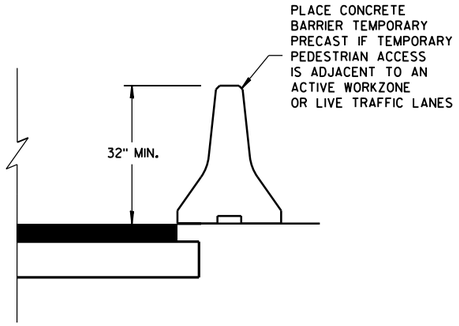
S.D.D. 15 D 30-3b

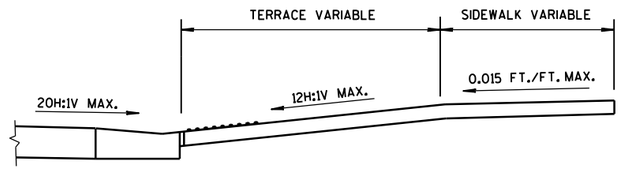




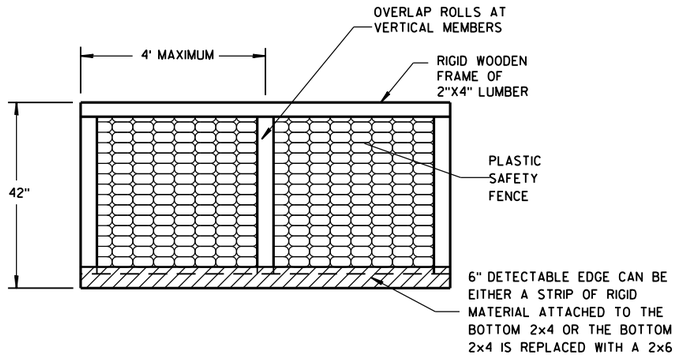
TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

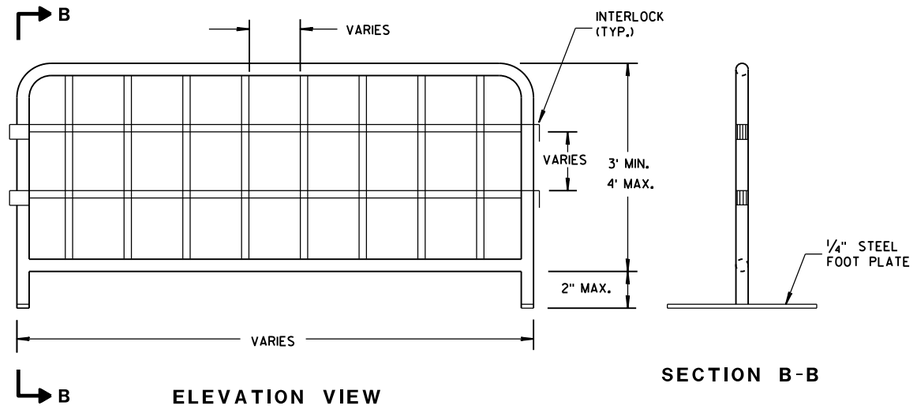
(1) INTERCHANGEABLE WITH THE PEDESTRIAN SAFETY FENCE.



SECTION A-A



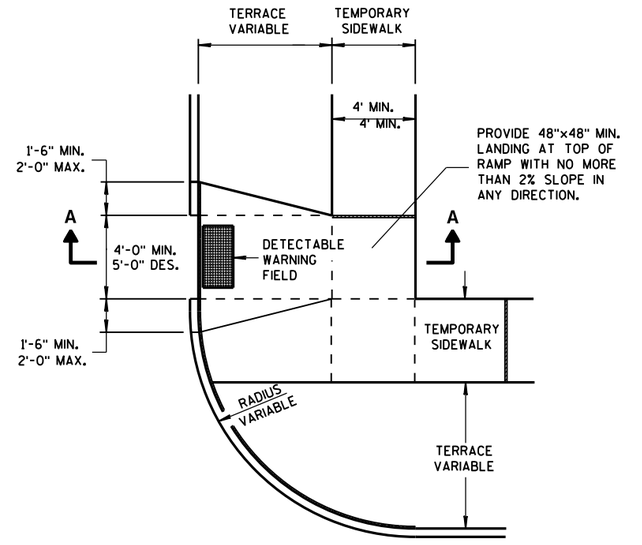
PEDESTRIAN SAFETY FENCE



ELEVATION VIEW

SECTION B-B

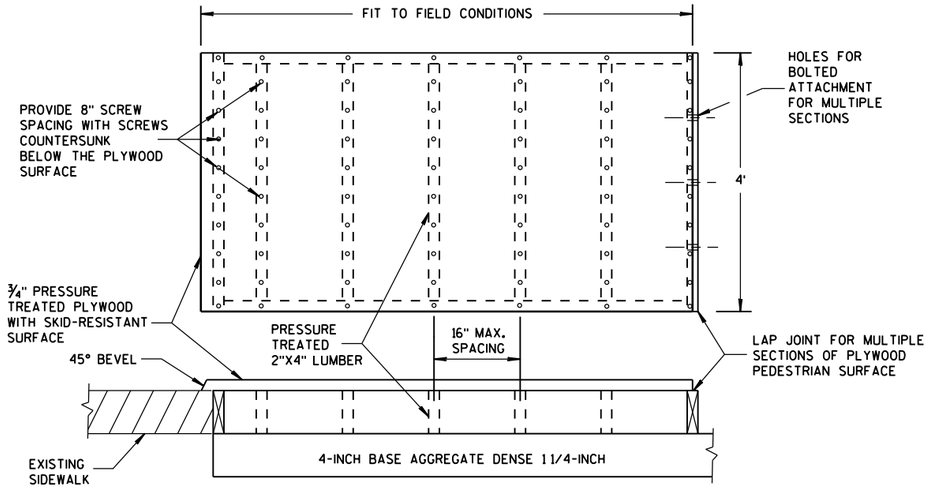
TEMPORARY PEDESTRIAN STEEL BARRICADE (1)



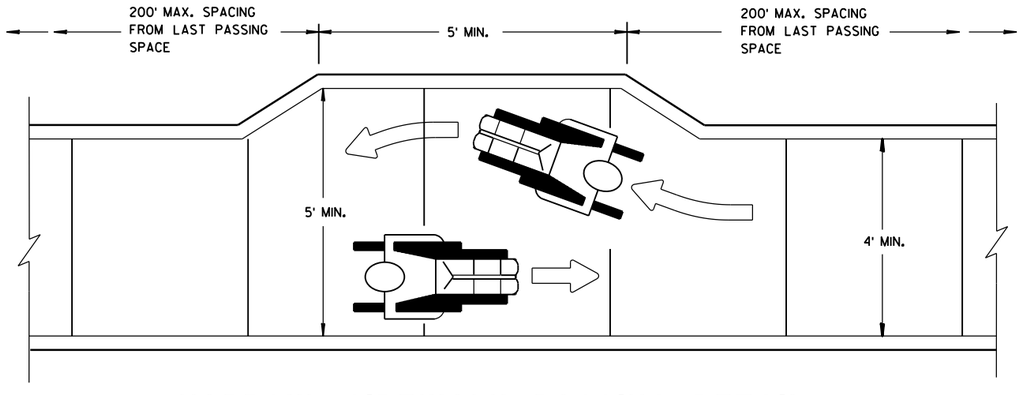
PLAN VIEW

TEMPORARY TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)



TEMPORARY PEDESTRIAN SURFACE PLYWOOD



NARROW SIDEWALK PASSING DETAIL

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016
DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZON SAFETY ENGIN IC
FHWA

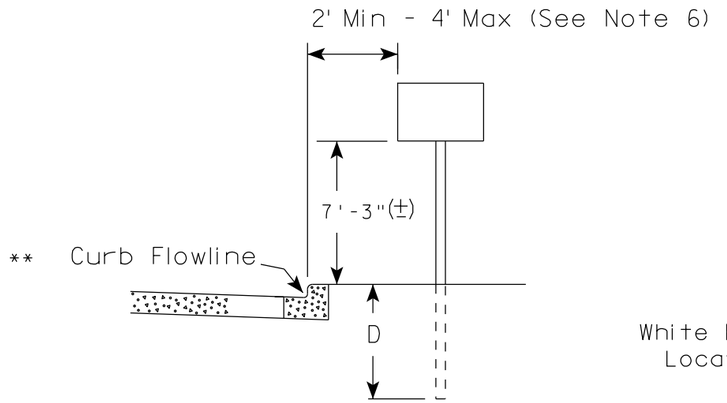
6

S.D.D. 15 D 30-3c

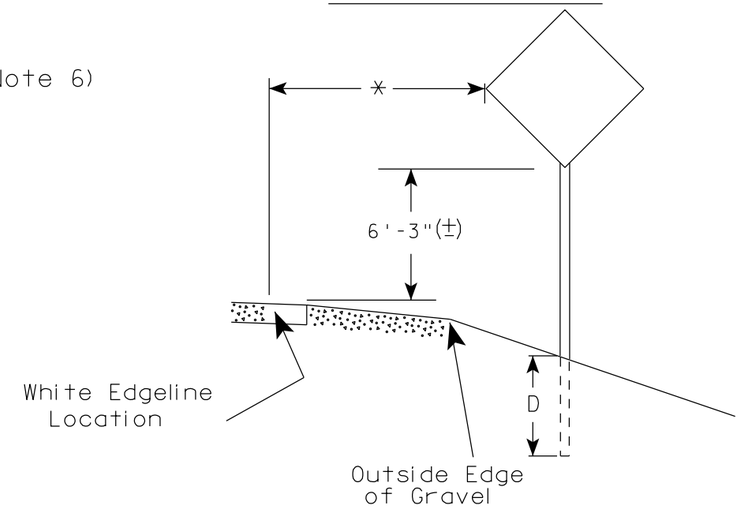
6

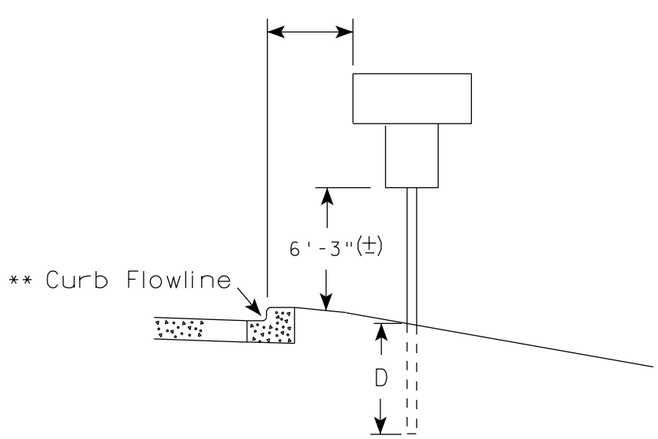
S.D.D. 15 D 30-3c

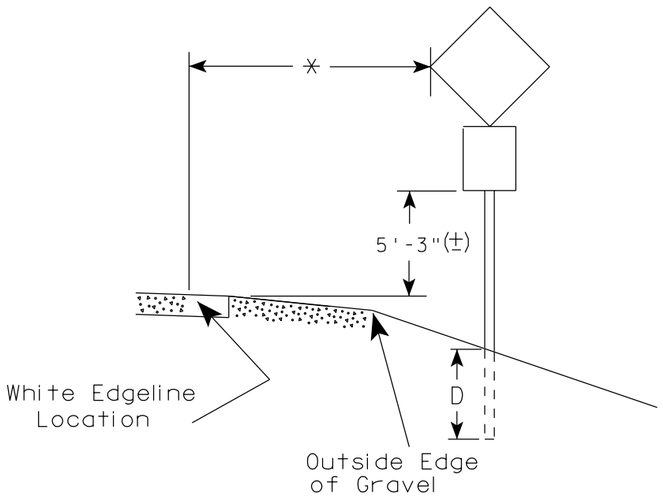
URBAN AREA



RURAL AREA (See Note 2)







GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

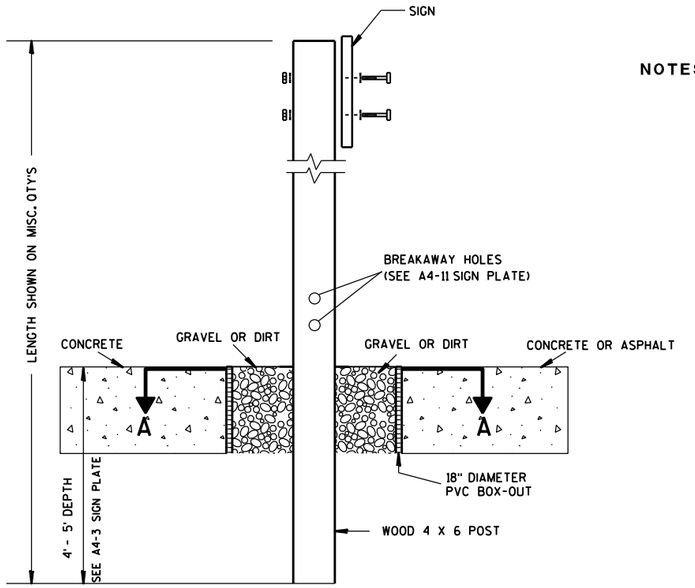
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-3.20

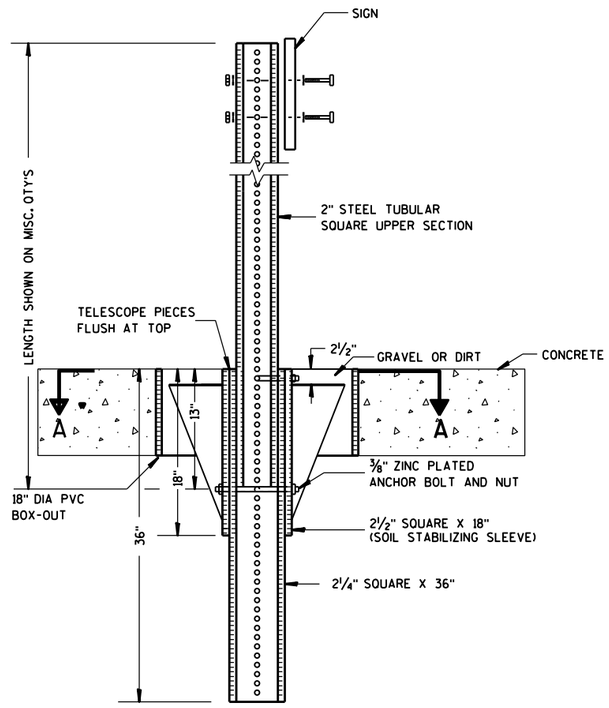
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



ELEVATION VIEW

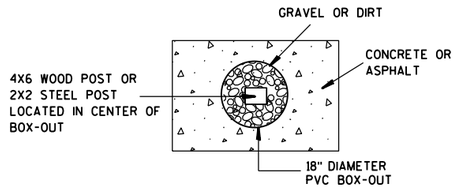
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

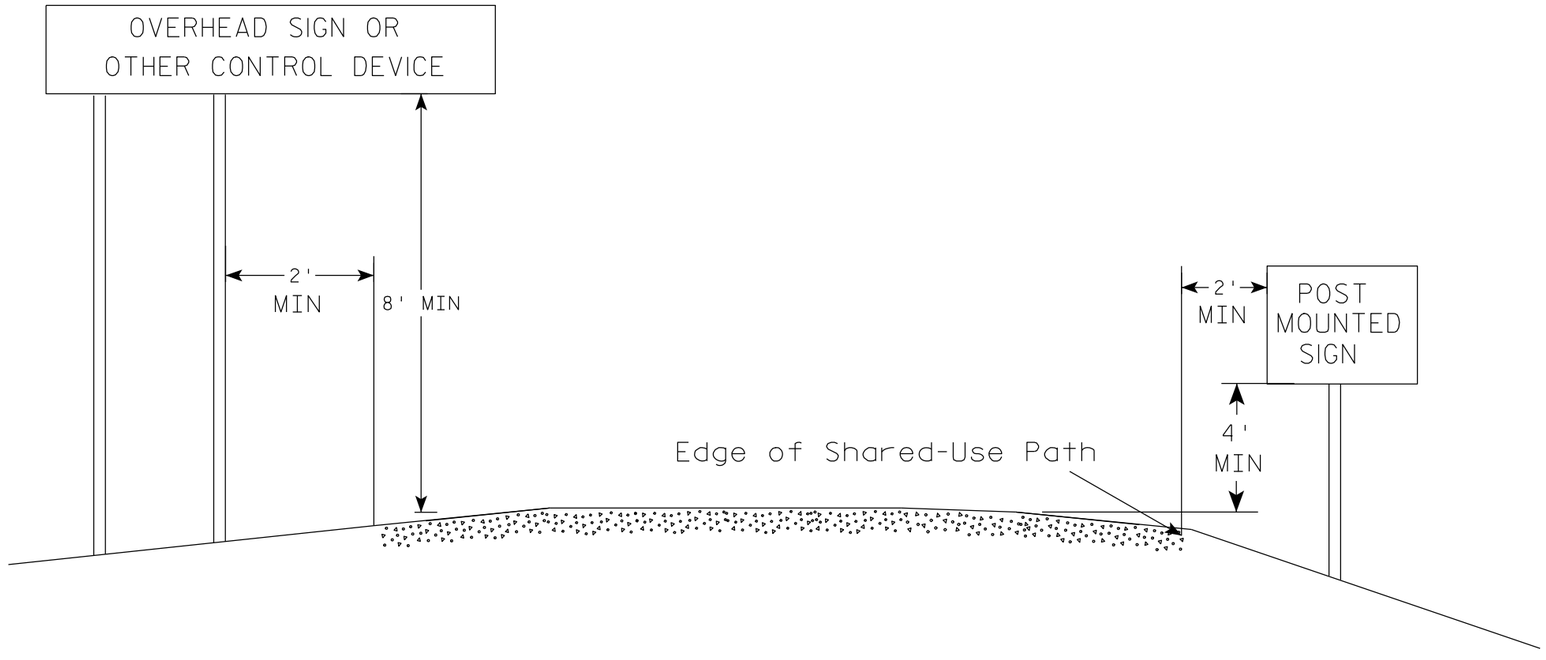
DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

GENERAL NOTES

1. Signs wider than 4 feet or larger than 20 sq. ft. shall be mounted on multiple posts. Refer to plate A4-4.
2. Offset distance shall be consistent with existing signs or consistent throughout length of project.



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON MULTI USE PATHS

WISCONSIN DEPT OF TRANSPORTATION

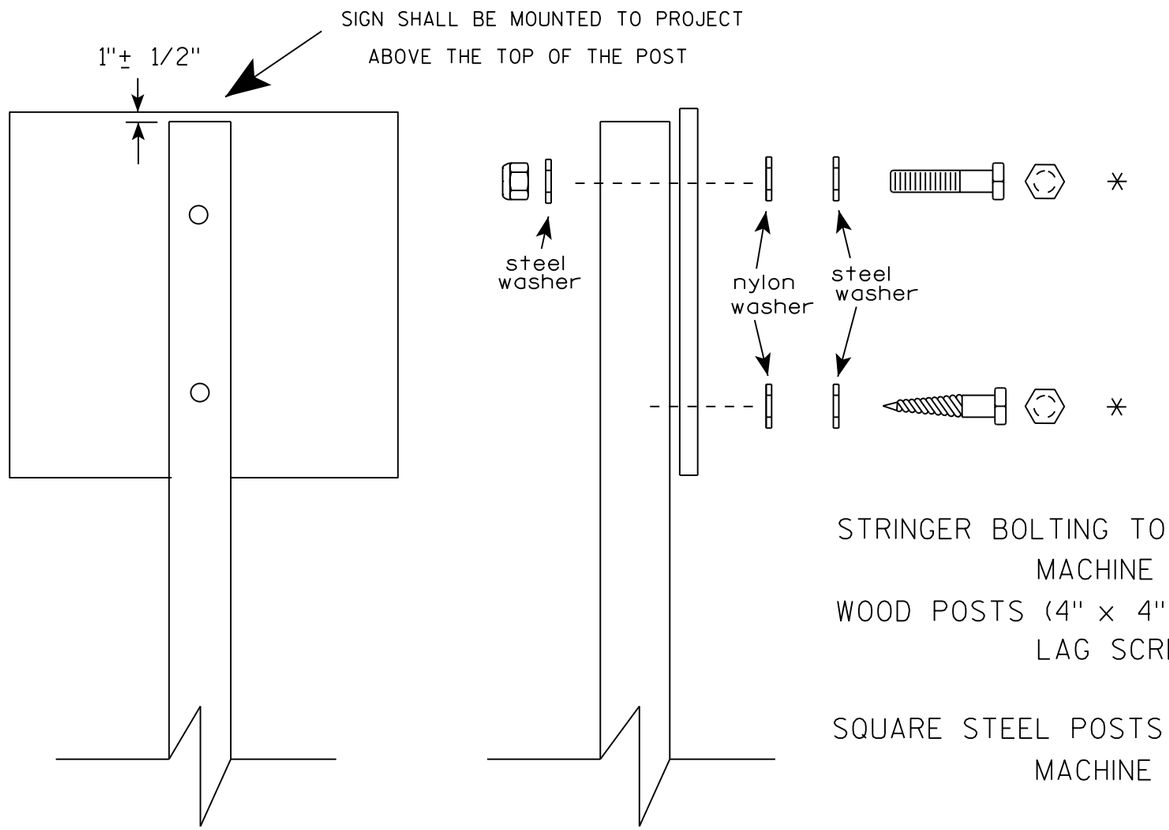
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/5/2012 PLATE NO. A4-3S.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 69	E

FILE NAME : c:\CAEFiles\Projects\tr_stdplate\A43S.DGN PLOT DATE : 05-MAR-2012 13:42 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 101.303739:1.000000

WISDOT/CADDS SHEET 42



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
WOOD POSTS (4" x 4" or 4" x 6")
LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)
SQUARE STEEL POSTS (2" x 2")
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
RIVETS - 9/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

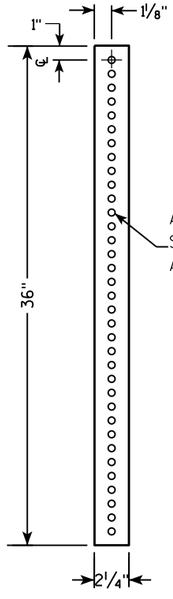
DATE 8/11/16 PLATE NO. A4-8.8

PROJECT NO: SHEET NO: E

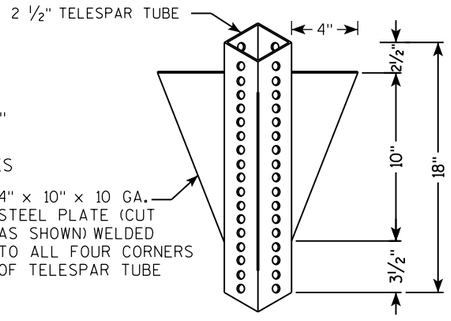
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A48.DGN PLOT DATE : 11-AUG-2016 11:35 PLOT BY : $$ plotuser $$

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

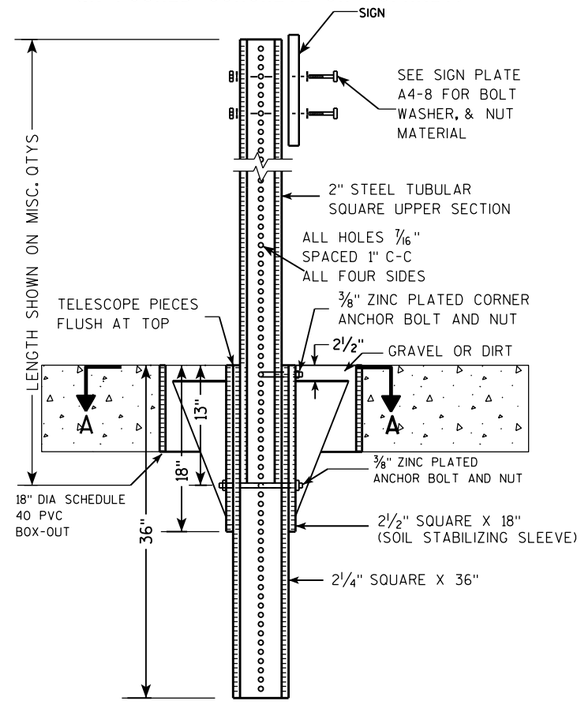
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



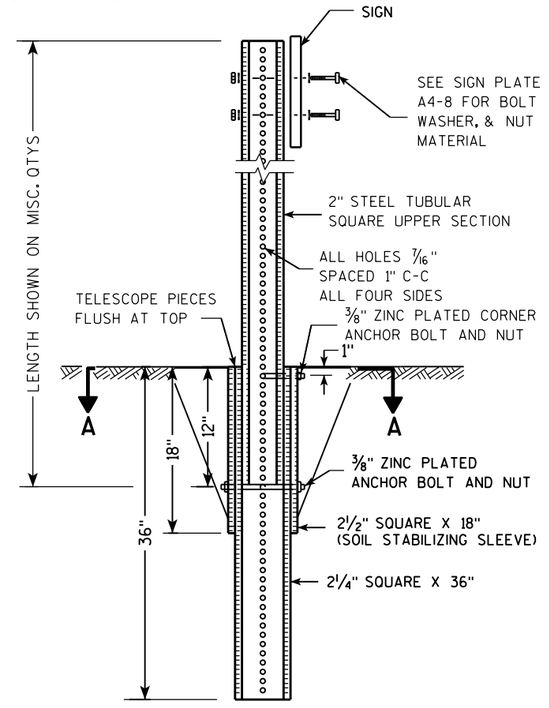
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

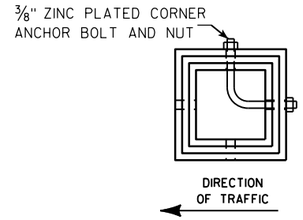


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

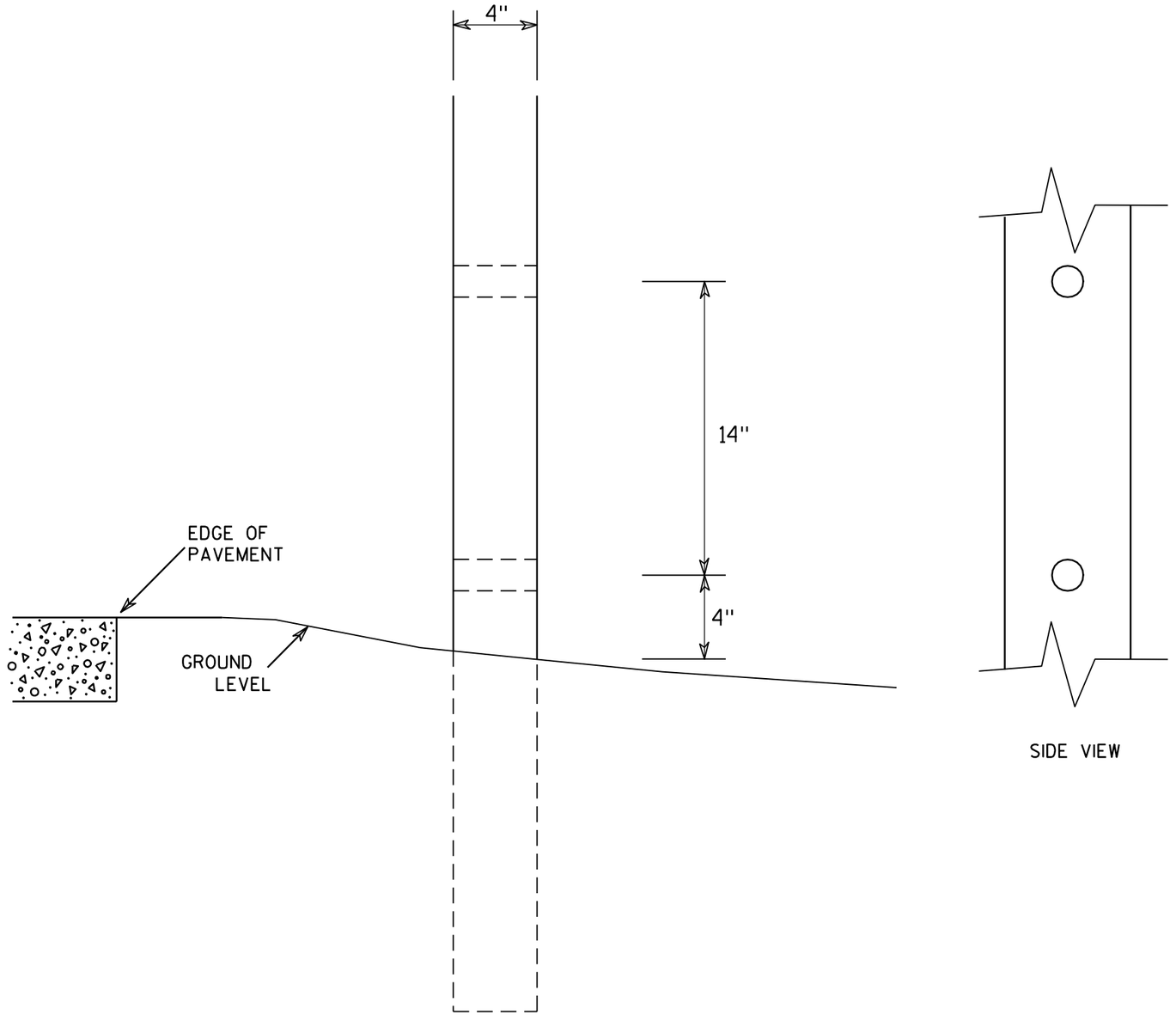
TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE 71 A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

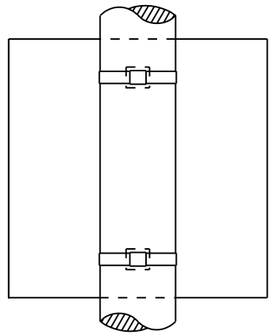
APPROVED Chester J Spang for State Traffic Engineer

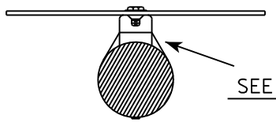
DATE 3/27/97 PLATE NO. A4-11.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 72	E

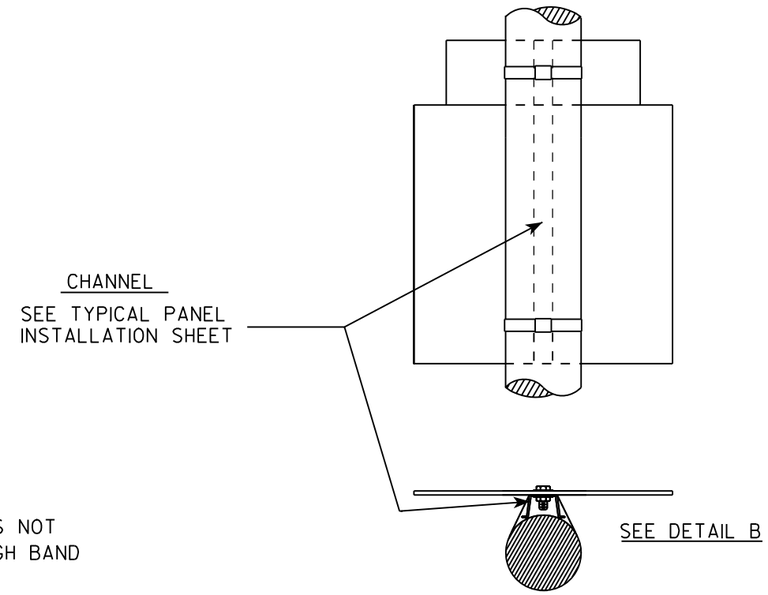
BANDING

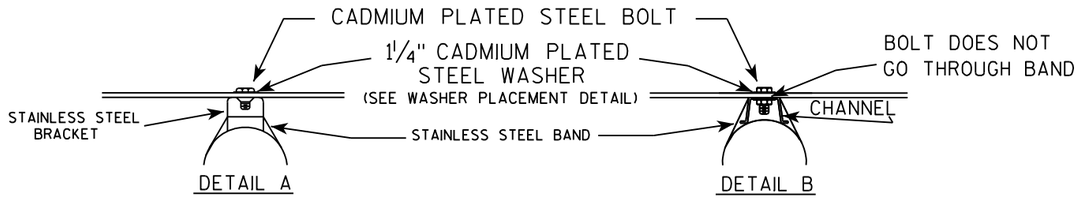
SINGLE SIGN



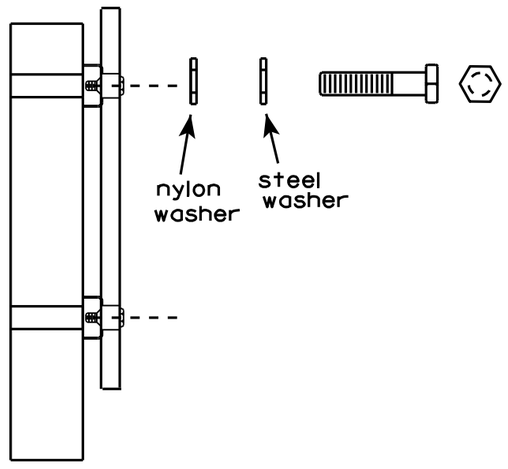


"J" ASSEMBLY





WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.

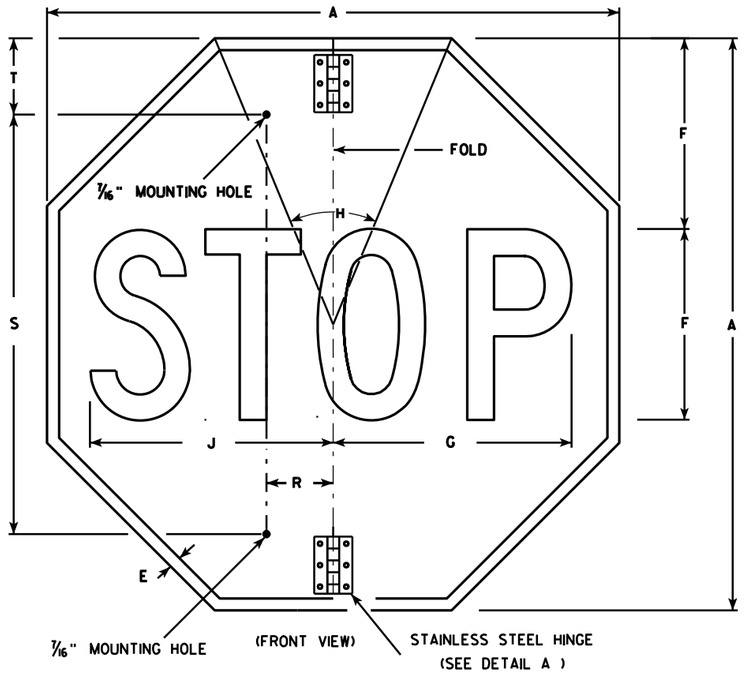
STANDARD SIGN
SIGN BANDING DETAILS

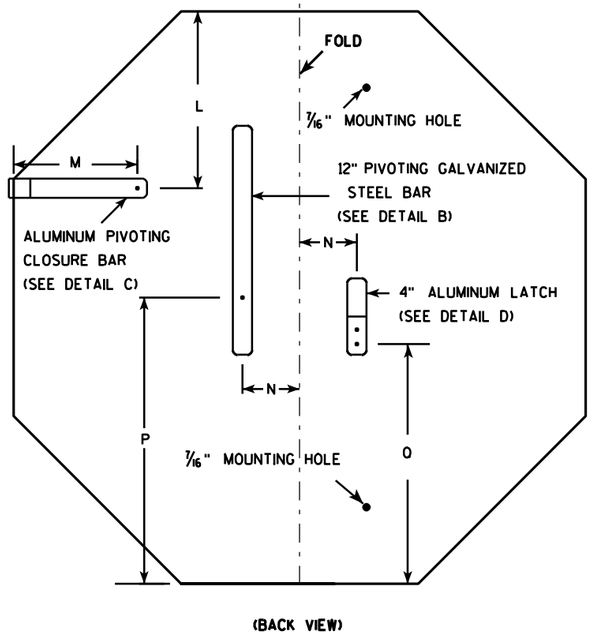
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/16/13 PLATE NO. A5-9.3

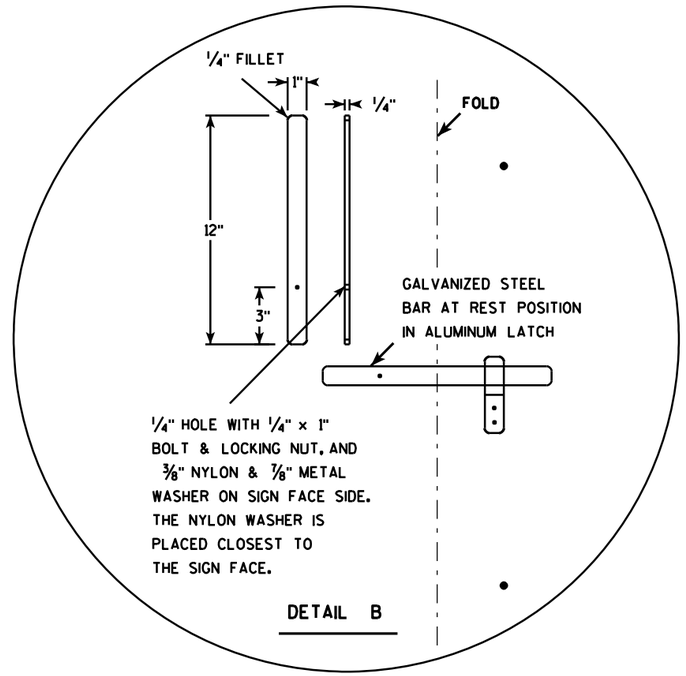
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 73	E

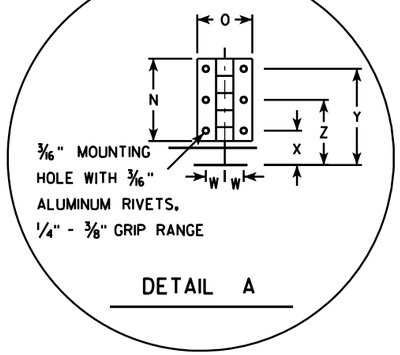


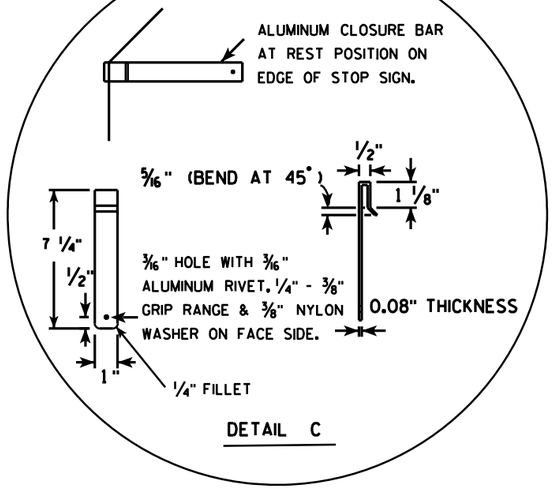


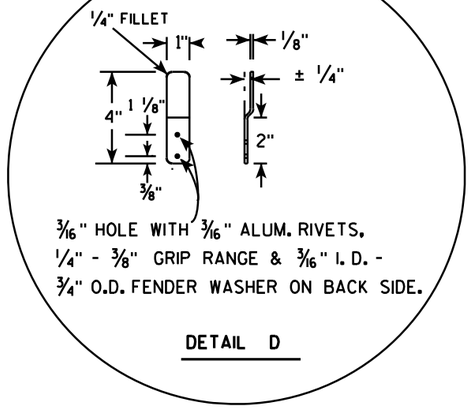
NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C
4. All hardware used on the folding STOP sign installation shall conform to 637.2.4 of the WIS DOT Standard Specification.









SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30				5/8	10	12 1/2	45		12 3/4		9 1/4	6 1/2	3	2	15	12 3/8	2 1/2	22	5			11/16	1 1/4	3 1/2	2 3/8	5.18
2M	36				3/4	12	15	45		15 3/8		11	6 1/2	3	2	18	15 3/8	2 1/2	26	5			11/16	1 1/4	3 1/2	2 3/8	7.46
3	36				3/4	12	15	45		15 3/8		11	6 1/2	3	2	18	15 3/8	2 1/2	26	5			11/16	1 1/4	3 1/2	2 3/8	7.46
4																											
5																											

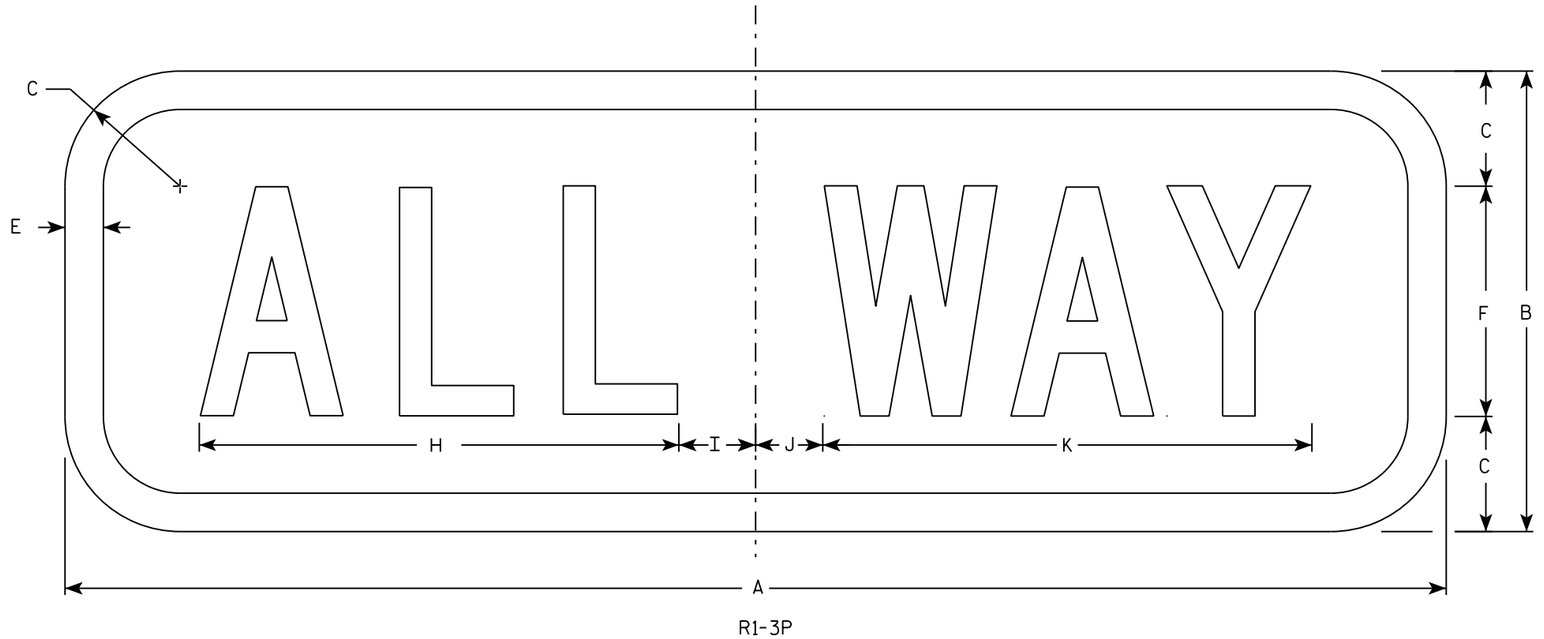
STANDARD SIGN
R1-1F

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1F.3

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 74 | E



R1-3P

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - Red
   Message - White
3. Message Series - C
4. For 30"x30" R1-1 use 18"x6" R1-3P sign
   For 36"x36" R1-1 use 24"x9" R1-3P sign
   For 48"x48" R1-1 use 30"x12" R1-3P sign

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	6	1 ½		½	3		6 ¼	1 ¼	⅞	6 ⅜																0.75
2S	18	6	1 ½		½	3		6 ¼	1 ¼	⅞	6 ⅜																1.5
2M	24	9	1 ½		½	5		9 ¼	1 ¼	¾	9 ¾																1.5
3	24	9	1 ½		½	5		9 ¼	1 ¼	¾	9 ¾																1.5
4	30	12	2 ¼		⅝	6		11	2 ¼	1 ½	11 ¾																2.5
5	30	12	2 ¼		⅝	6		11	2 ¼	1 ½	11 ¾																2.5

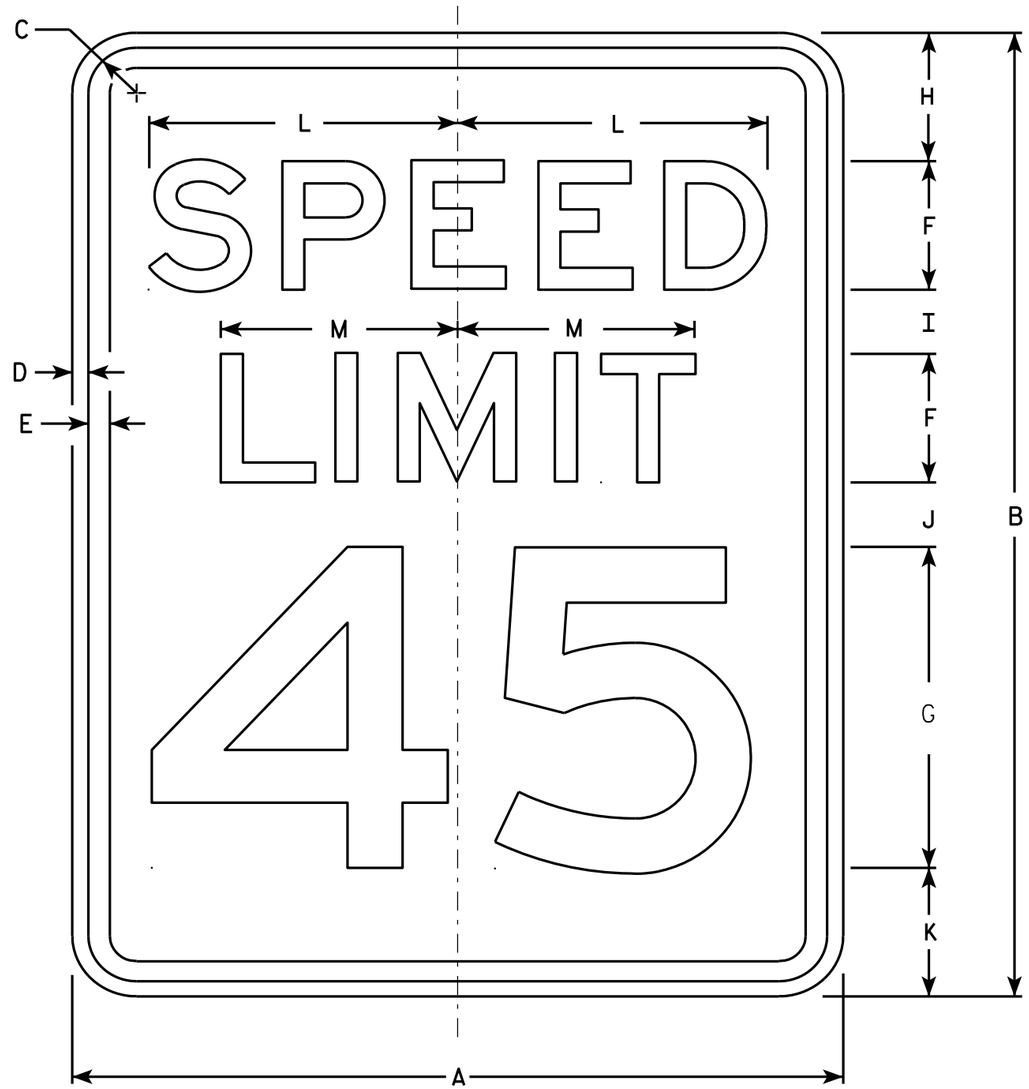
STANDARD SIGN
R1-3P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/29/16 PLATE NO. R1-3P.3

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 75 | E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

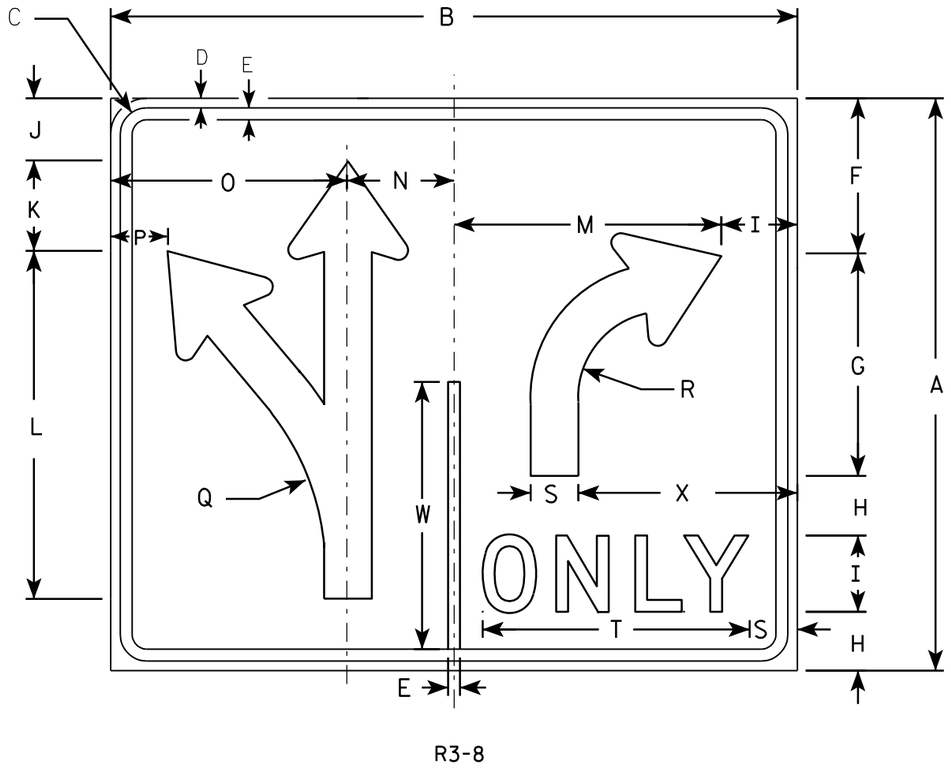
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

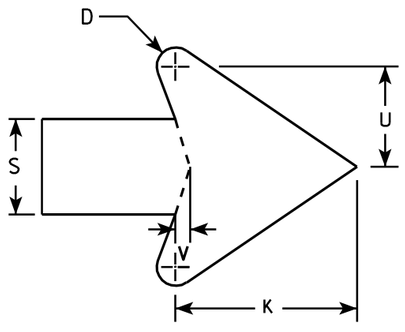
DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 76 | E

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R21.DGN PLOT DATE : 28-MAY-2010 08:32 PLOT BY : ditjph PLOT NAME : PLOT SCALE : 4.717577:1.000000
WISDOT/CADDS SHEET 42



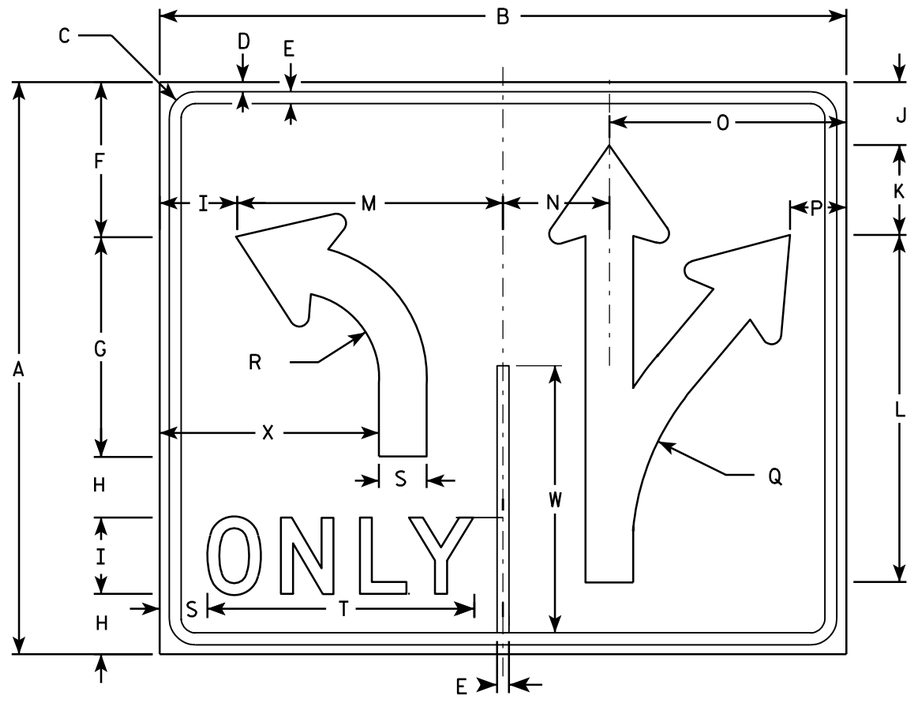
R3-8



ARROW DETAIL

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - WHITE
   Message - BLACK
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R3-8A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3/8	1/2	5/8	8 1/8	11 5/8	3 1/8	4	3 1/4	4 3/4	18 1/4	14	5 5/8	12 3/8	3	13 1/4	4 1/2	2 1/2	14	2 5/8	3/8	14	11 1/2			7.5
2M	30	36	1 3/8	1/2	5/8	8 1/8	11 5/8	3 1/8	4	3 1/4	4 3/4	18 1/4	14	5 5/8	12 3/8	3	13 1/4	4 1/2	2 1/2	14	2 5/8	3/8	14	11 1/2			7.5
3																											
4	48	54	2 1/4	3/4	1	13 1/4	18 1/2	5 1/8	6	5 1/4	7 1/8	29 1/8	21	8 3/8	18 5/8	4 3/8	21 7/8	7 1/4	3 3/4	20 5/8	4	5/8	22 3/8	17 1/4			18.0
5	48	54	2 1/4	3/4	1	13 1/4	18 1/2	5 1/8	6	5 1/4	7 1/8	29 1/8	21	8 3/8	18 5/8	4 3/8	21 7/8	7 1/4	3 3/4	20 5/8	4	5/8	22 3/8	17 1/4			18.0

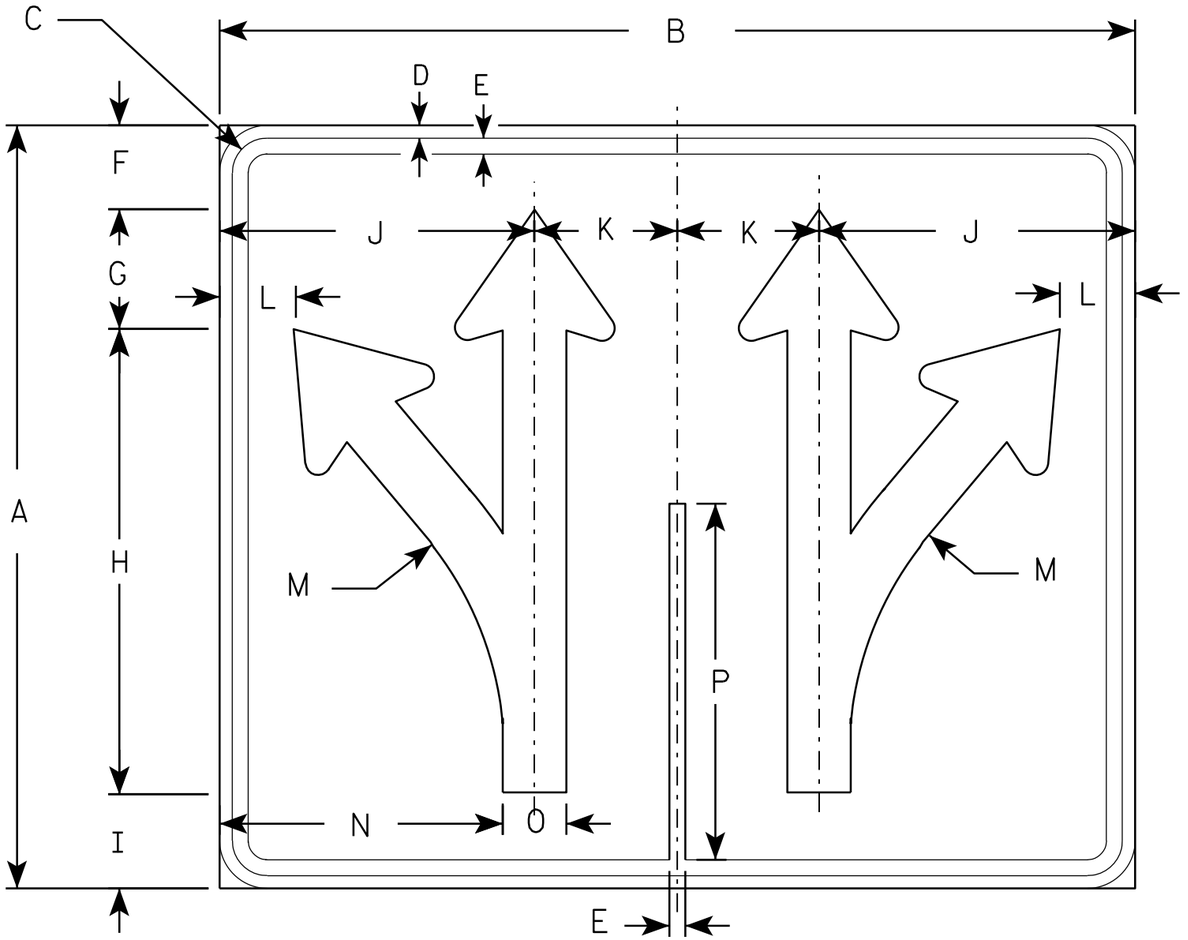
STANDARD SIGN
R3-8 & R3-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/2011 PLATE NO. R3-8.5

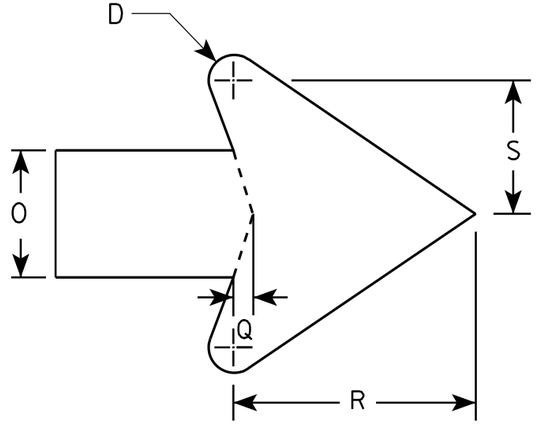
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 77	E



R3-8K

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - WHITE
   Message - BLACK
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3/8	1/2	5/8	3 1/4	4 3/4	18 1/4	3 3/4	12 3/8	5 5/8	3	13 1/4	11 1/8	2 1/2	14	3/8	4 3/4	2 5/8								7.5
2M	30	36	1 3/8	1/2	5/8	3 1/4	4 3/4	18 1/4	3 3/4	12 3/8	5 5/8	3	13 1/4	11 1/8	2 1/2	14	3/8	4 3/4	2 5/8								7.5
3																											
4	48	54	2 1/4	3/4	1	5 1/4	7 1/2	29 1/4	5 1/8	18 5/8	8 3/8	4 3/8	22 1/4	16 3/4	3 3/4	22 3/8	5/8	7 1/8	4								18.0
5	48	54	2 1/4	3/4	1	5 1/4	7 1/2	29 1/4	5 1/8	18 5/8	8 3/8	4 3/8	22 1/4	16 3/4	3 3/4	22 3/8	5/8	7 1/8	4								18.0

STANDARD SIGN
R3-8K

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

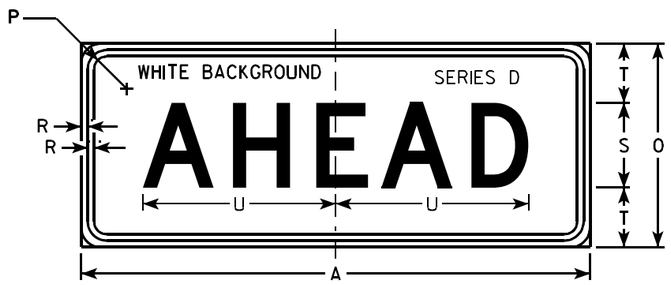
DATE 3/21/2011 PLATE NO. R3-8K.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 78	E

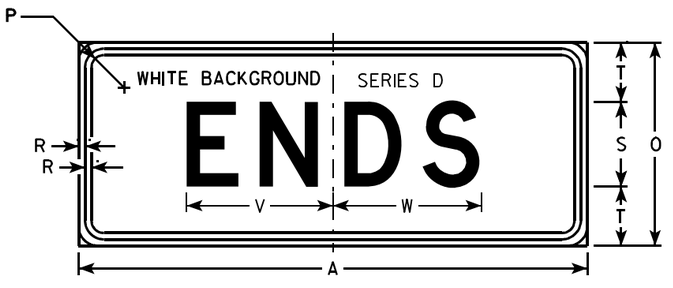
R3-17

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - AS SHOWN
   Message - BLACK
3. Message Series - C or as noted on the Signs.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R3-17ap



R3-17bp

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	R3-17 Area sq. ft.	R3-17ap Area sq. ft.	R3-17bp Area sq. ft.
1																													
2S	30	24	1 1/8	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/8	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
2M	30	24	1 1/8	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/8	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
3																													
4																													
5																													

STANDARD SIGN
R3-17 & R3-17a&bp

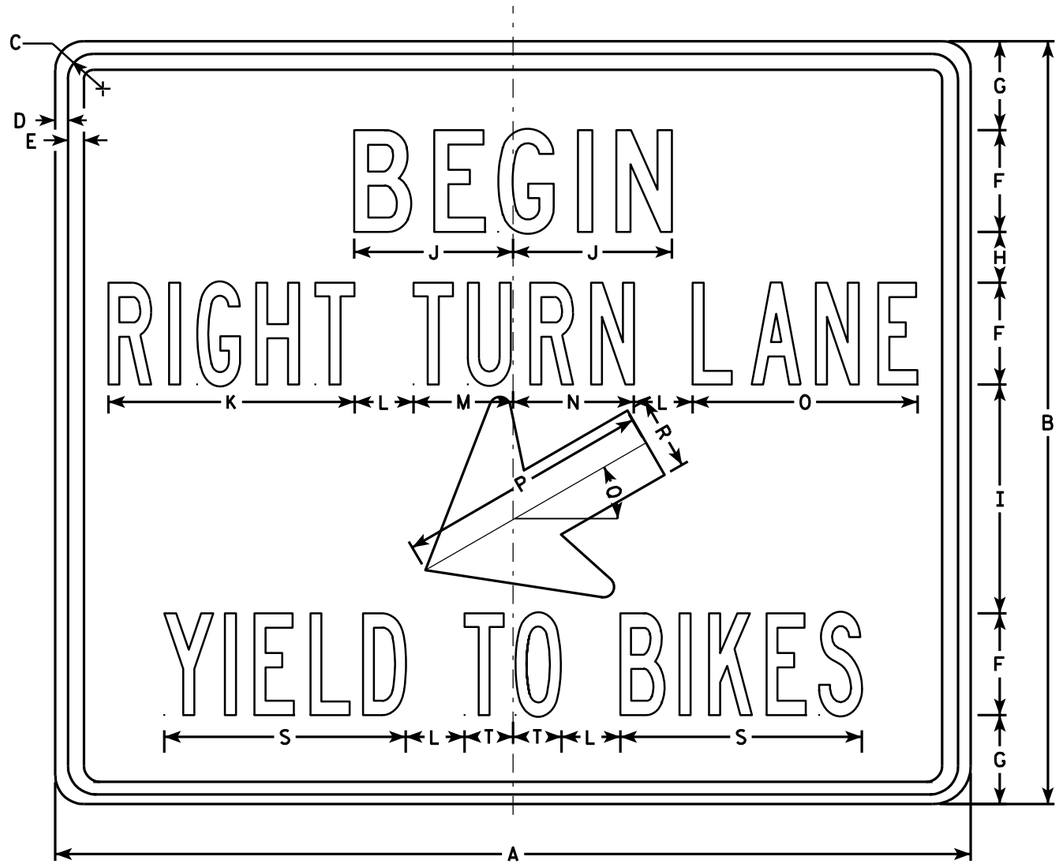
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/12/2011 PLATE NO. R3-17.2

PROJECT NO:

SHEET NO: 79 E



R4-4

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series C
   Lines 2 & 3 are Series B

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	30	1 3/8	1/2	5/8	4	3 1/2	2	9	6 1/4	9 3/4	2 3/8	3 7/8	4 3/4	8 7/8	10	30°	2 7/8	9 1/2	1 7/8							7.5
2M	36	30	1 3/8	1/2	5/8	4	3 1/2	2	9	6 1/4	9 3/4	2 3/8	3 7/8	4 3/4	8 7/8	10	30°	2 7/8	9 1/2	1 7/8							7.5
3																											
4																											
5																											

STANDARD SIGN
R4-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-4.2

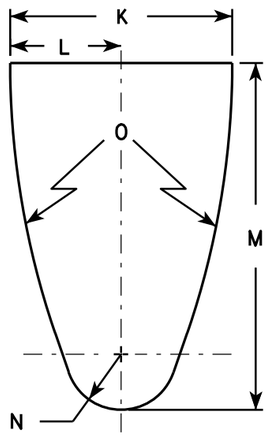
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 80 | E

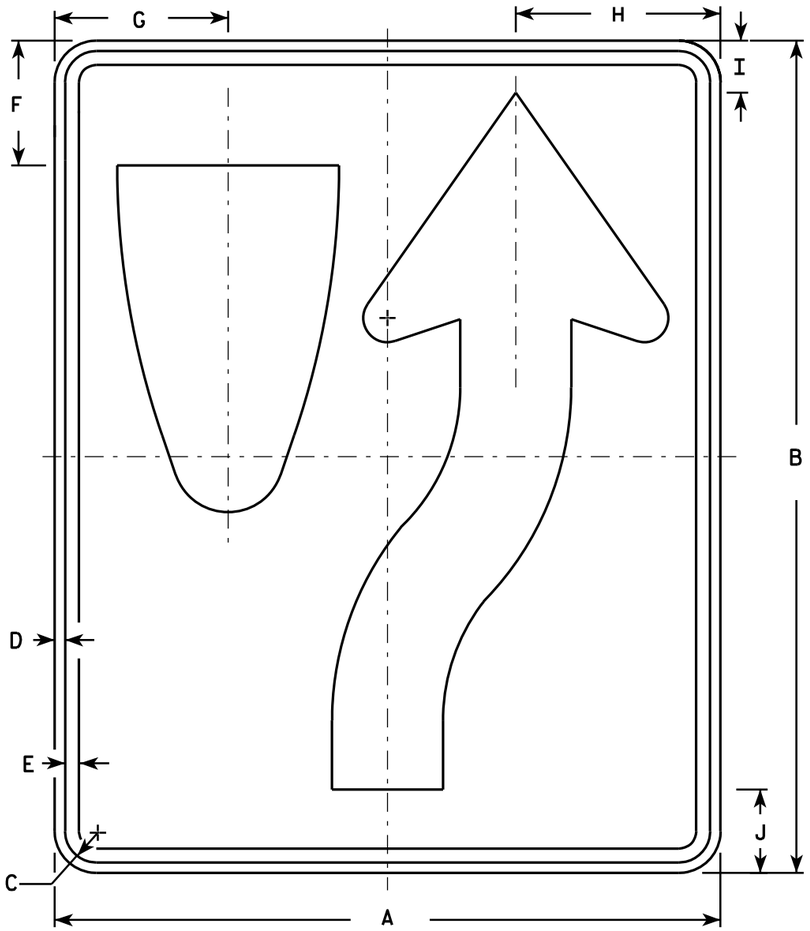
FILE NAME : C:\Users\PROJECTS\tr_stdplate\R44.DGN PLOT DATE : 25-MAR-2011 13:45 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 5.959043:1.000000

WISDOT/CADDS SHEET 42

7

7

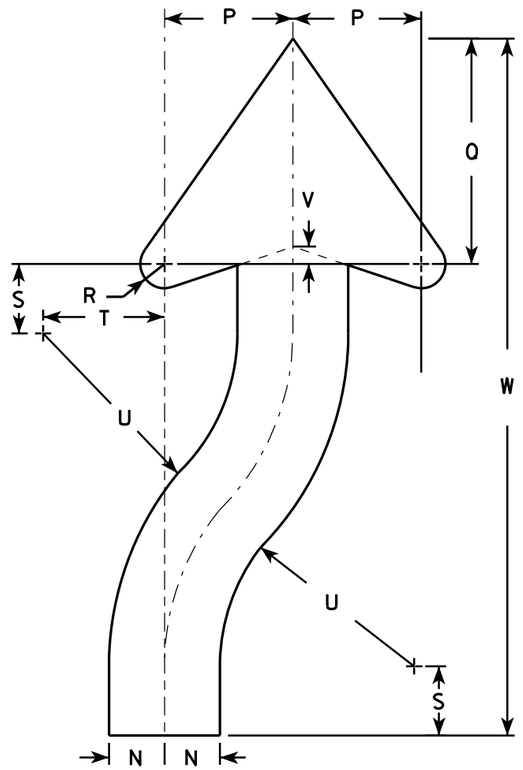




R4-7

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
2. Color:
   Background - White
   Message - Black
3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
4. R4-8 is the same as R4-7 except Legend is reversed.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3 3/8	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5/8	1 7/8	3 1/4	6 3/4	1/2	20 3/8				3.0
2S	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2M	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 5/8	5	8 3/4	18	1 1/4	50 1/4				20.0

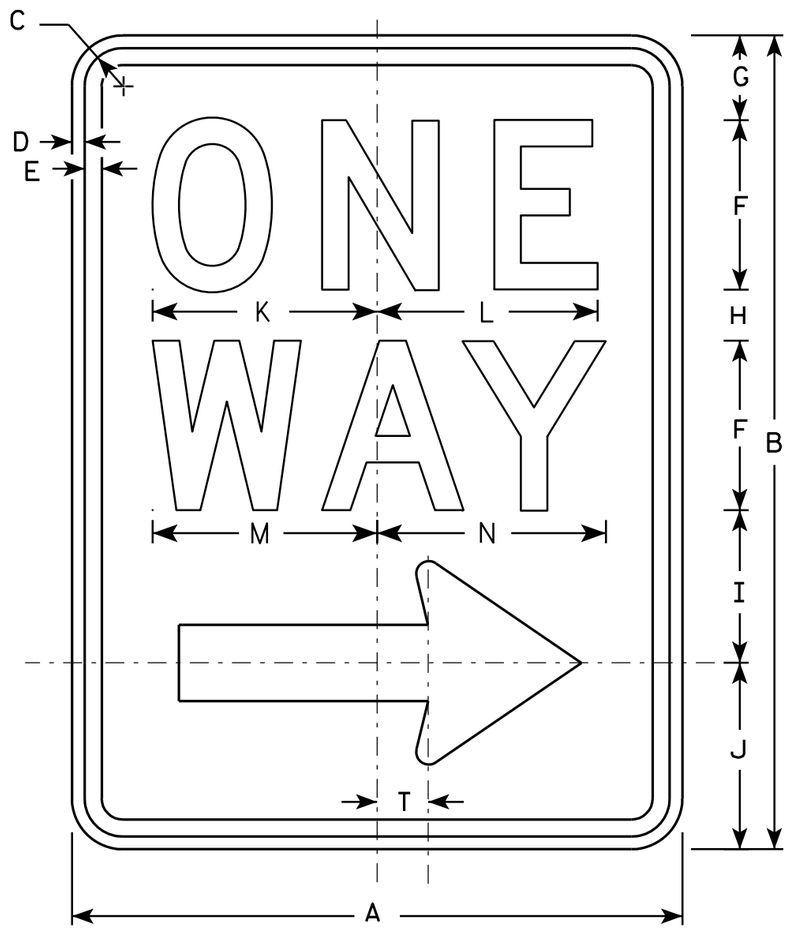
STANDARD SIGN
R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-7.8

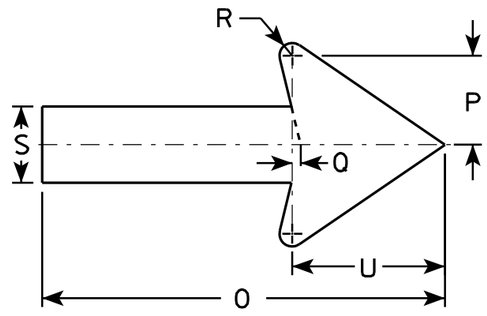
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 81	E



R6-2R

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R6-2L same as R6-2R except arrow points to the left.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z
1	18	24	1 1/8	3/8	1/2	5	2 1/2	1 1/2	4 1/2	5 1/2	6 5/8	6 1/2	6 5/8	6 3/4	11 7/8	2 5/8	1/4	3/8	2 1/4	1 1/2	4 1/2					
2S	24	30	1 1/8	3/8	1/2	6	3	2 1/2	5 1/2	7	8 1/8	8 1/8	8 1/2	8 5/8	16	3 1/2	3/8	1/2	3	2	6					
2M	30	36	1 3/8	1/2	5/8	8	2 1/2	2 5/8	6 7/8	8	10 1/2	10 1/2	11 1/4	11 1/4	20	4 3/8	1/2	5/8	3 3/4	2 1/2	7 1/2					
3	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
4	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
5																										

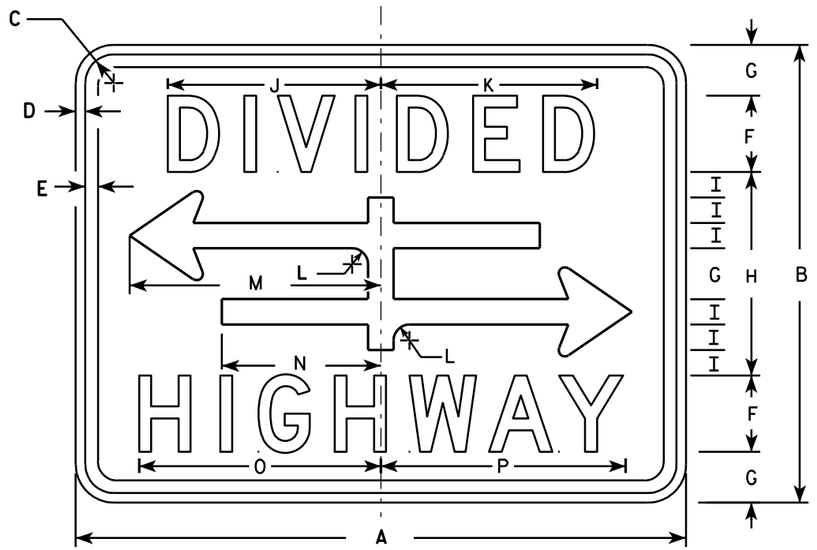
STANDARD SIGN
R6-2 R&L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/2/10 PLATE NO. R6-2.8

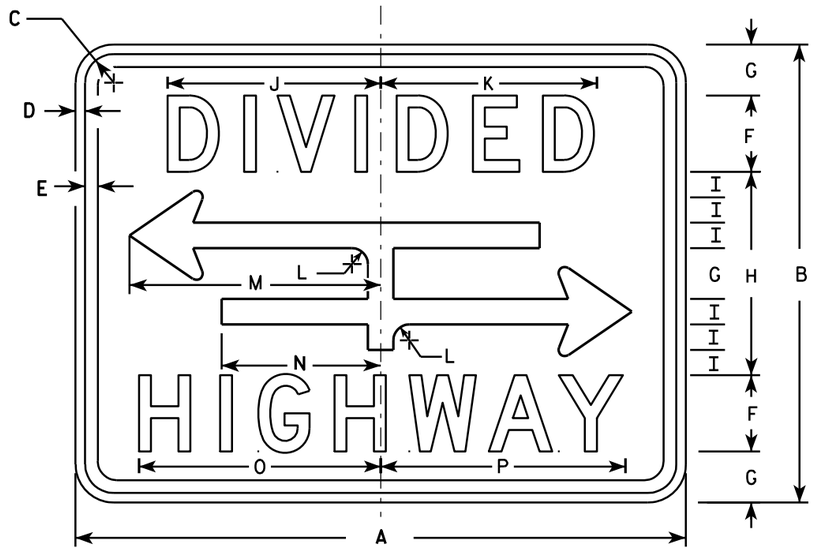
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 82	E



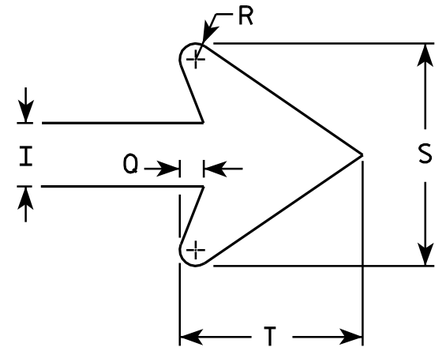
R6-3

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R6-3A



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	18	1 1/8	3/8	3/8	3	2	8	1	8 3/8	8 1/2	5/8	9 7/8	6 1/4	9 1/2	9 5/8	3/8	1/4	3 1/2	2 3/4							3.0
2S	30	24	1 1/8	3/8	1/2	4	2 5/8	10 3/4	1 3/8	10 1/2	10 5/8	7/8	12 1/2	7 7/8	12 1/4	12 3/8	1/2	3/8	4 5/8	3 5/8							5.0
2M	30	24	1 1/8	3/8	1/2	4	2 5/8	10 3/4	1 3/8	10 1/2	10 5/8	7/8	12 1/2	7 7/8	12 1/4	12 3/8	1/2	3/8	4 5/8	3 5/8							5.0
3																											
4																											
5																											

STANDARD SIGN
R6-3 & R6-3A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R6-3.5

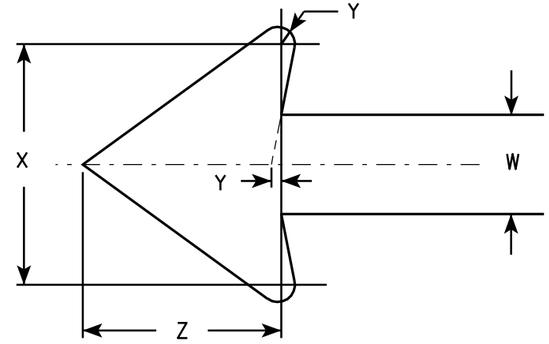
PROJECT NO:

SHEET NO: 83 E

R7-51

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R7-51D (double arrow)
   R7-51R (right arrow)
   R7-51L (left arrow)
6. Lines 1, 3 and 4 are Series C.
   Line 2 is Series B.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	4 7/8	5/8	1 3/4	2 1/2	4 3/8	3 7/8	3/4	1 3/4	1/8	1 1/2	1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	5 3/4	1 1/8	1 1/2	3 1/8	5 1/2	5 7/8	1 1/8	2 5/8	1/4	2 1/4	3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	7 1/8	1 1/4	2	3 3/4	6 1/2	7 3/4	1 1/2	3 1/2	1/4	3	5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	7 1/8	1 1/4	2	3 3/4	6 1/2	7 3/4	1 1/2	3 1/2	1/4	3	5.0
4																											
5																											

STANDARD SIGN
R7-51

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

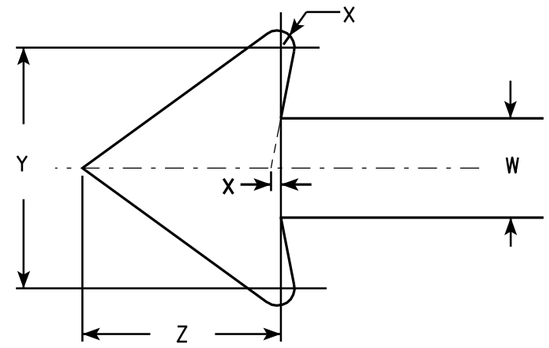
DATE 3/31/2011 PLATE NO. R7-51.6

PROJECT NO:	HWY:	COUNTY:			SHEET NO: 84	E

R7-53

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-53D (double arrow)
   R7-53L (left arrow)
   R7-53R (right arrow)



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 5/8	1 3/4	1/4	4 1/2	2 3/8	3 7/8	3/4	1/8	1 3/4	1 1/2	1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	3 3/8	1 1/2	5/8	5 3/8	3	5 7/8	1 1/8	1/4	2 5/8	2 1/4	3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	4	2	5/8	6 5/8	3 5/8	7 3/4	1 1/2	1/4	3 1/2	3	5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	4	2	5/8	6 5/8	3 5/8	7 3/4	1 1/2	1/4	3 1/2	3	5.0
4																											
5																											

STANDARD SIGN
R7-53

WISCONSIN DEPT OF TRANSPORTATION

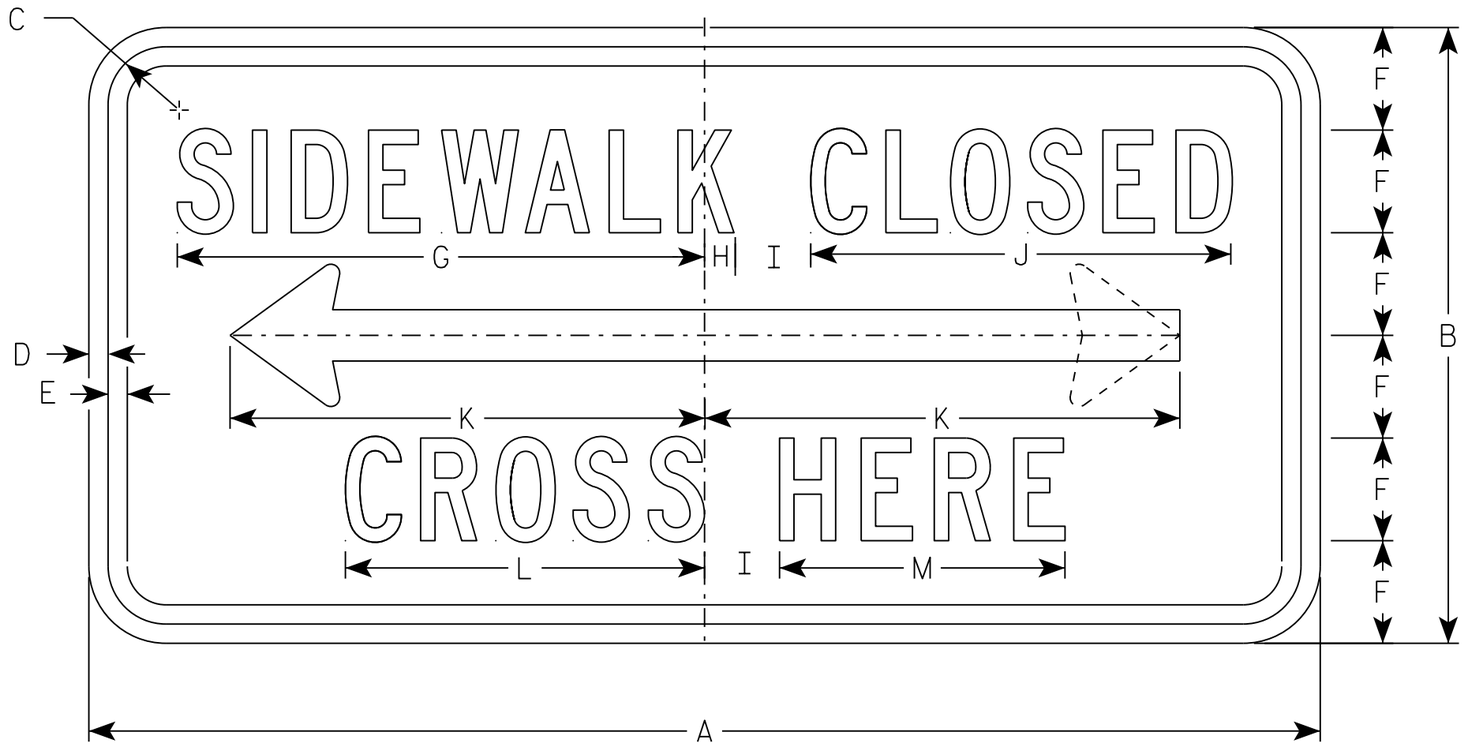
APPROVED Matthew R Rauch
for State Traffic Engineer

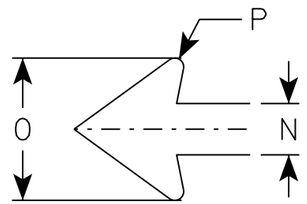
DATE 3/31/2011 PLATE NO. R7-53.6

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 85	E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for paths and Trails.





R9-11A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	2	10 1/4	5/8	1 1/2	8 1/4	9 1/4	7	5 5/8	1	2 3/4	1/8											2.0
2M	24	12	1 1/8	3/8	3/8	2	10 1/4	5/8	1 1/2	8 1/4	9 1/4	7	5 5/8	1	2 3/4	1/8											2.0
3	30	15	1 1/8	3/8	1/2	2	13	3/4	2	10 1/4	12 3/8	8 7/8	6 7/8	1 1/4	3 5/8	1/4											3.125
4																											
5																											

STANDARD SIGN
R9-11A

WISCONSIN DEPT OF TRANSPORTATION

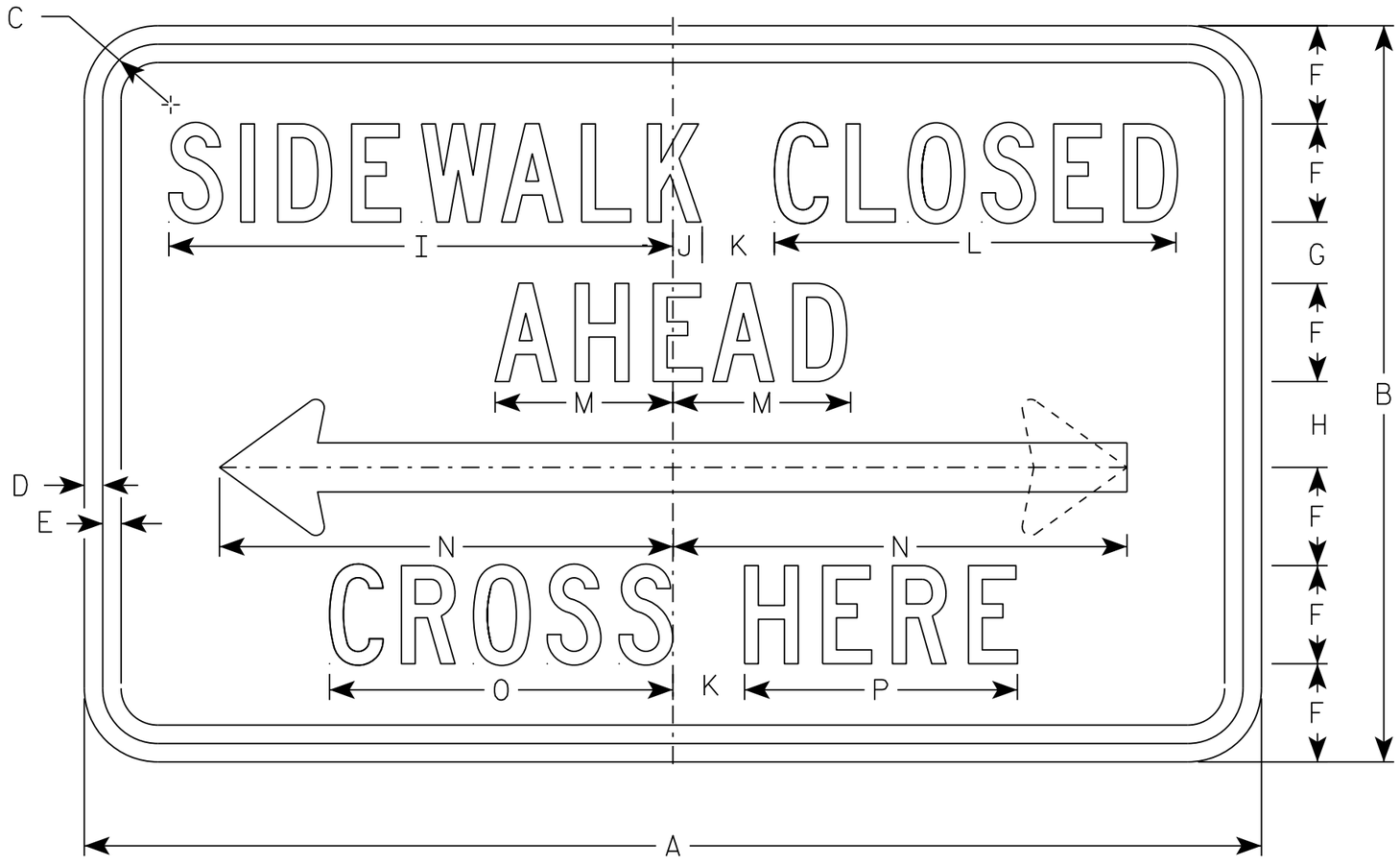
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/29/16 PLATE NO. R9-11A.3

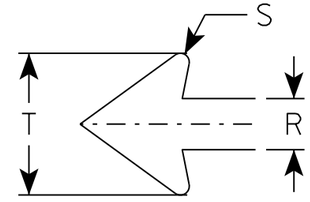
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 86 E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - C except Size 1 is Series D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-11



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 5/8	3 1/2	9 1/4	6 5/8	5 1/8		1	1/8	2 3/4							2.0
2M	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 5/8	3 1/2	9 1/4	6 5/8	5 1/8		1	1/8	2 3/4							2.0
3	30	15	1 1/8	3/8	1/2	2	1 1/2	1 1/2	13	3/4	2	10 1/4	4 5/8	12 3/8	8 7/8	6 7/8		1 1/4	1/4	3 5/8							3.125
4																											
5																											

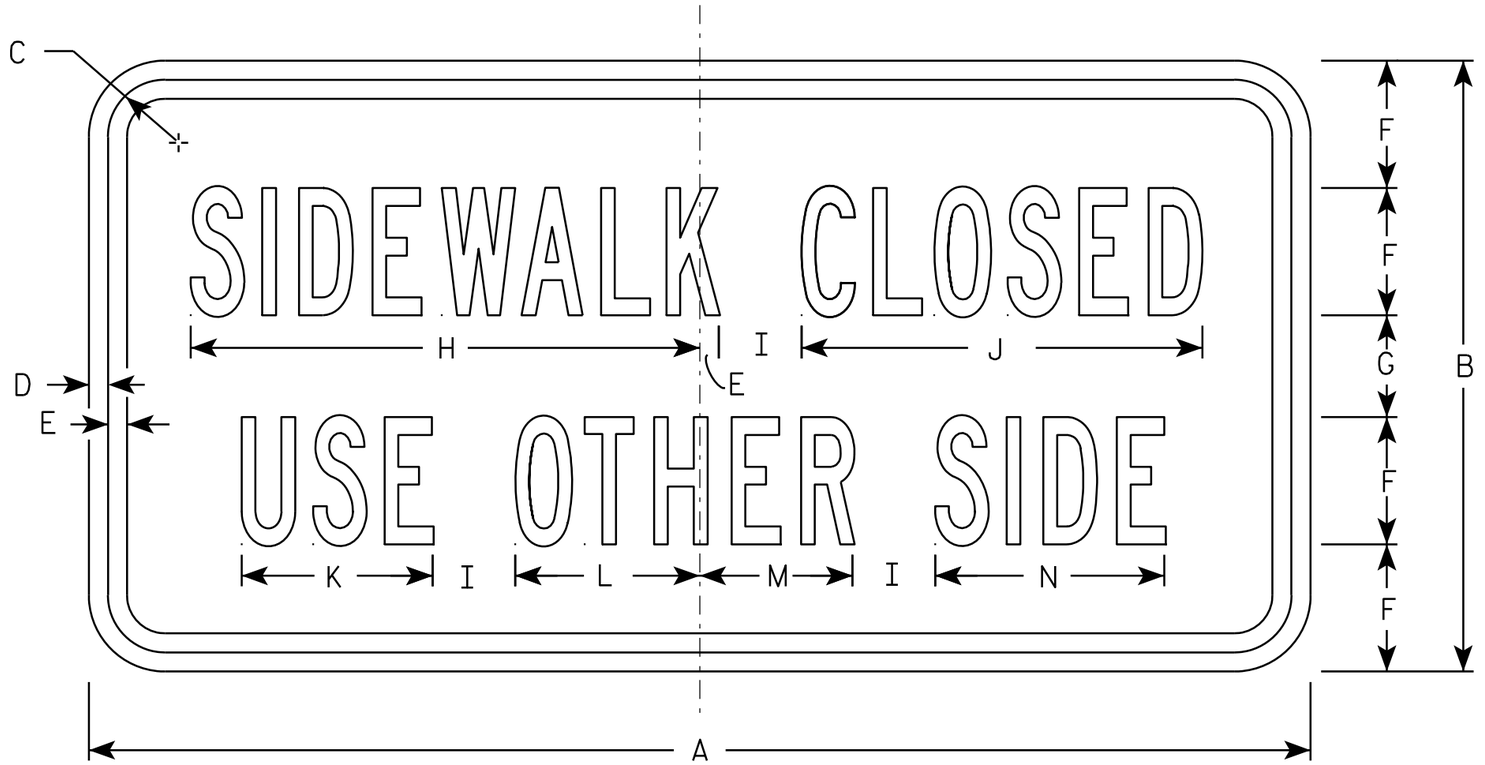
STANDARD SIGN
R9-11

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/29/16 PLATE NO. R9-11.3

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 87	E



R9-10

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	12	1 1/8	3/8	3/8	2 1/2	2	10	1 5/8	7 7/8	3 3/4	3 5/8	3	4 1/2													2.0
2S	48	24	2 3/4	3/4	3/4	5	4	20	3 1/4	15 3/4	7 1/2	7 1/4	6	9													8.0
2M	48	24	2 3/4	3/4	3/4	5	4	20	3 1/4	15 3/4	7 1/2	7 1/4	6	9													8.0
3																											
4																											
5																											

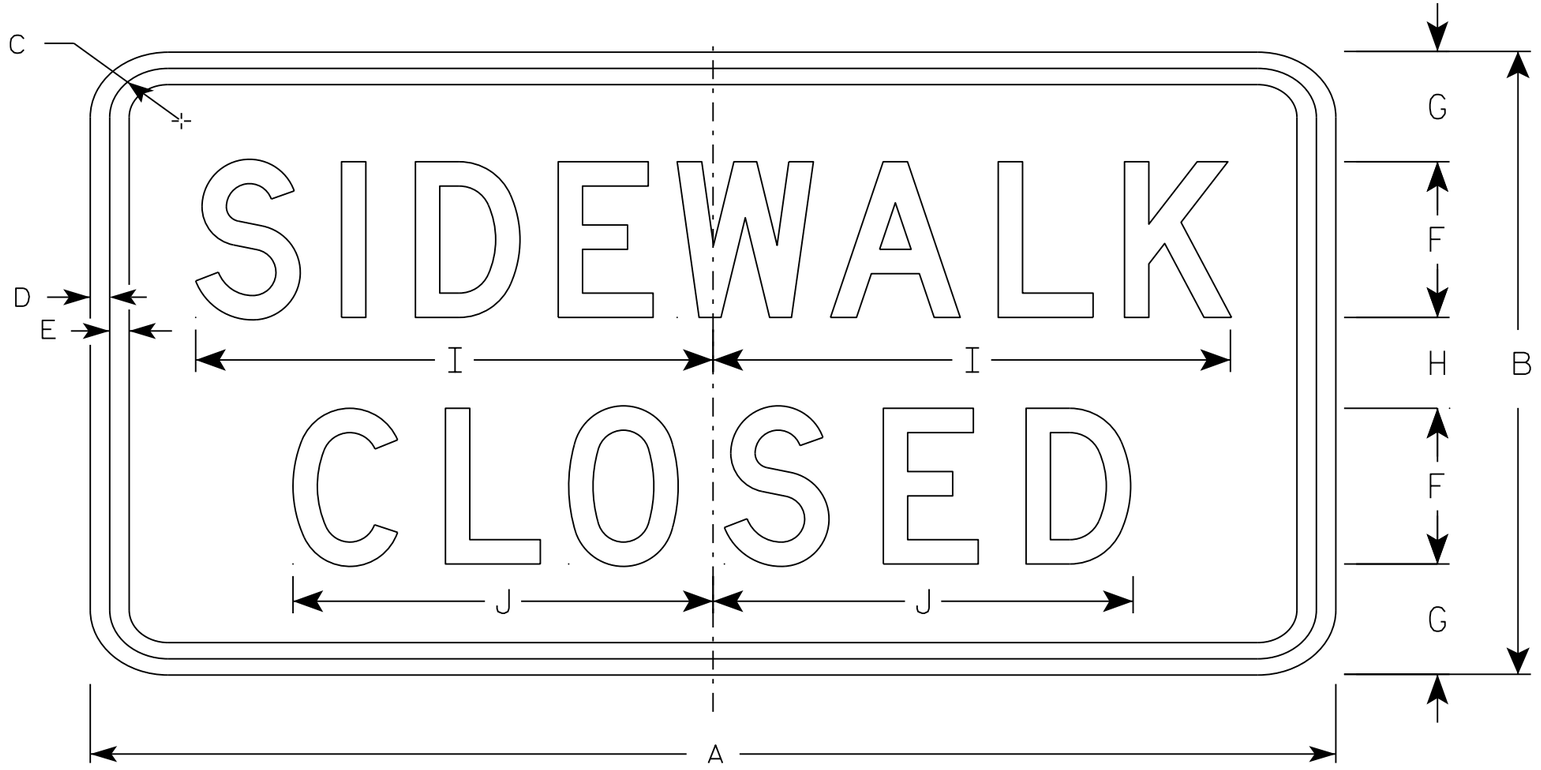
STANDARD SIGN
R9-10

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/16/2012 PLATE NO. R9-10.5

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 88	E



R9-9

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/11/16 PLATE 89 R9-9.6

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

EARTHWORK

BRAUND ST

STATION	DISTANCE	END AREA CUT (SF)	INCREMENTAL CUT (CY)	CUMULATIVE CUT (CY)
11+46	0	120.4	0	0
12+00	54	97.8	217.3	218
13+00	100	113.3	390.9	609
13+50	50	63.8	164.0	773
14+00	50	62.3	116.7	890
15+00	100	62.3	230.9	1121
16+00	100	62.3	230.9	1352
17+00	100	62.3	230.9	1583
18+00	100	62.3	230.9	1814
19+00	100	62.3	230.9	2045
20+00	100	62.3	230.9	2276
20+86	86	148.8	336.2	2613
Multi-Use Path	---	---	10.0	2623

PH

STATION	DISTANCE	END AREA CUT (SF)	INCREMENTAL CUT (CY)	CUMULATIVE CUT (CY)
101+78	0	53.8	0.0	0
102+00	22	53.8	43.9	44
103+00	100	53.8	199.4	244
104+00	100	53.8	199.4	444
105+00	100	53.8	199.4	644
106+00	100	53.8	199.4	844
107+00	100	53.8	199.4	1044
108+00	100	53.8	199.4	1244
109+00	100	53.8	199.4	1444
110+00	100	53.8	199.4	1644
111+00	100	53.8	199.4	1844
112+00	100	53.8	199.4	2044
EBS*	300	38.0	422.2	2467

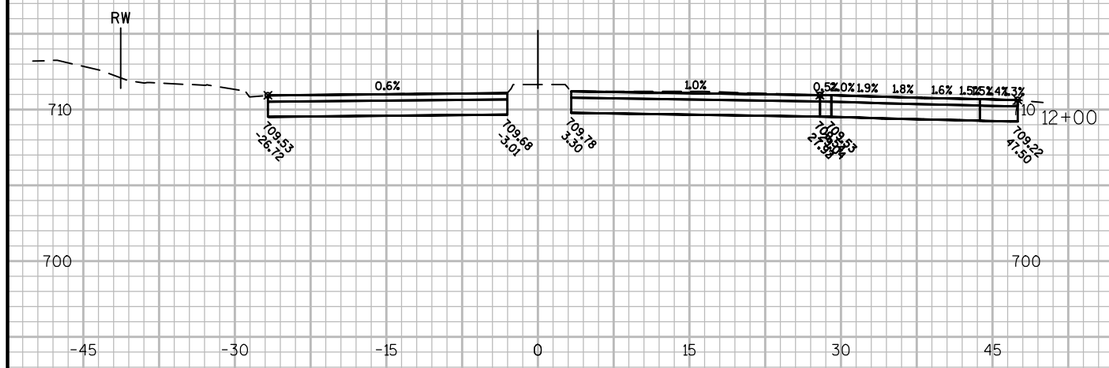
*EBS Estimated from STA 108+50 to 111+50.

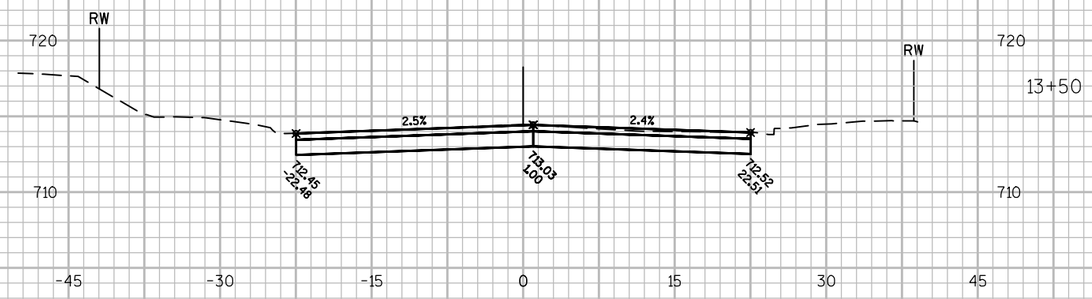
Total 5090

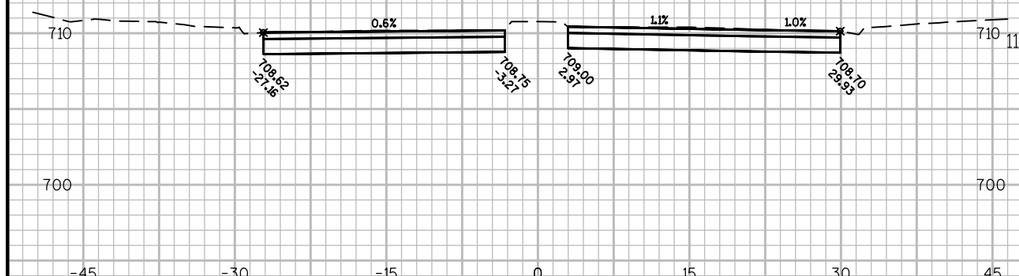
9

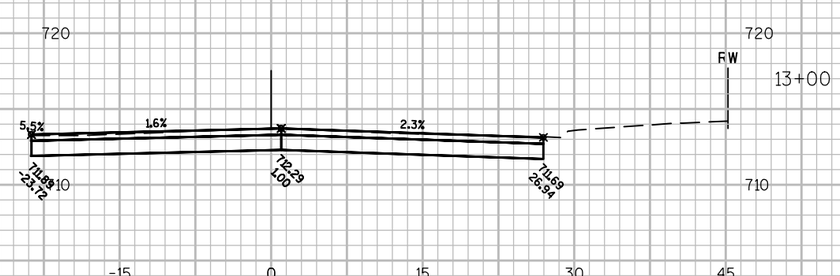
9

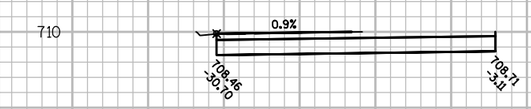
FILE NAME : \\SEHLX1\PROJECTS\KO\O\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090101-EW-BRAUNDPH.DWG
LAYOUT NAME - 090101-ew
PLOT DATE : 7/21/2017 10:41 AM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.
WISDOT/CADDS SHEET 49

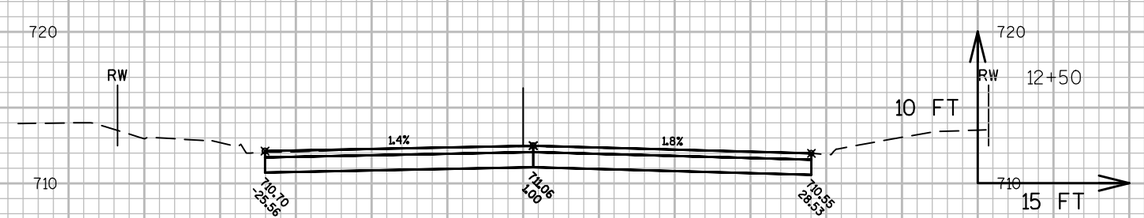












9

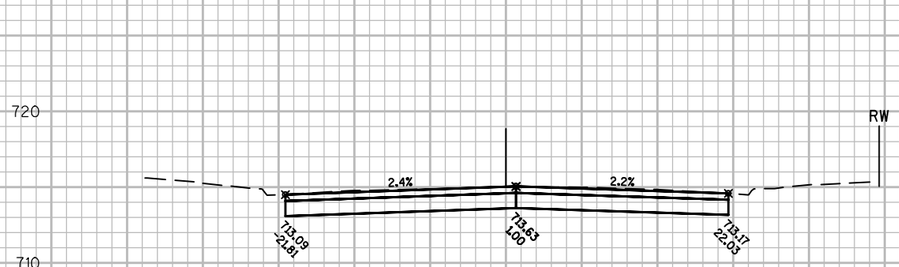
9

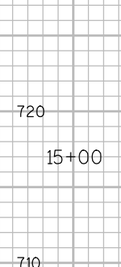
PROJECT NO: 5991-02-54	HWY: BRAUND STREET	COUNTY: LA CROSSE	CROSS SECTIONS: BRAUND STREET	SHEET 91	E

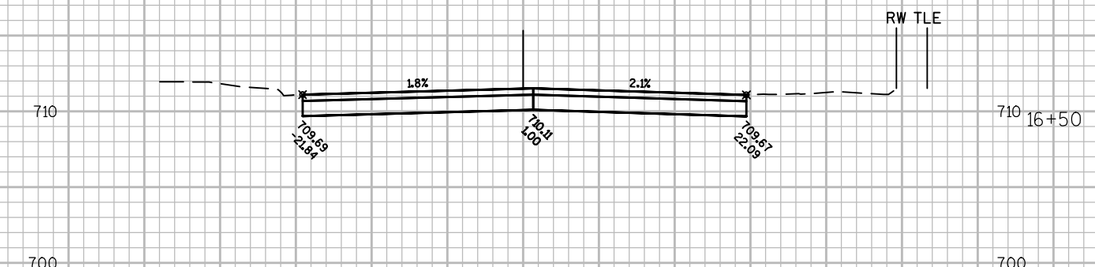
FILE NAME : \\SEHLX1\PROJECTS\KO\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-BRAUND.DWG
LAYOUT NAME - 090201-XS-BRAUND - 0902 - (1)

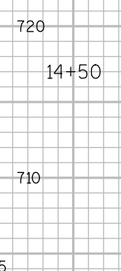
PLOT DATE : 1/3/2017 3:45 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:15 FT

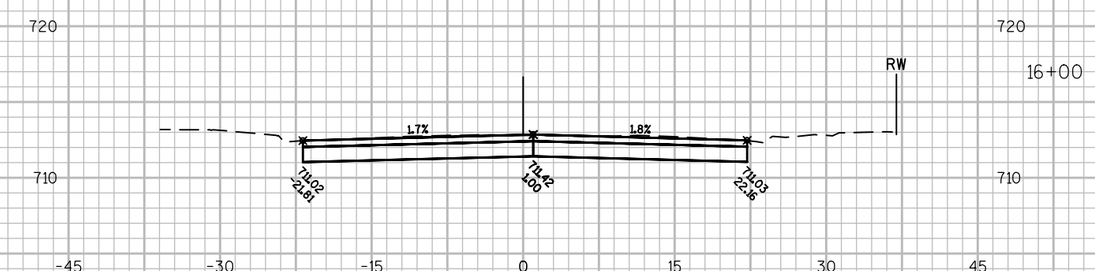
WISDOT/CADDS SHEET 49

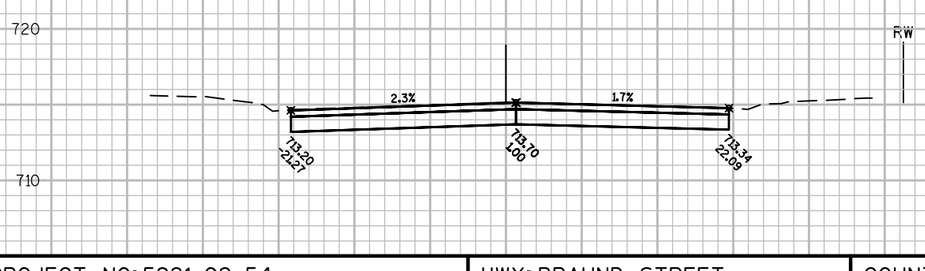


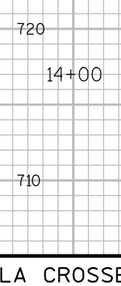


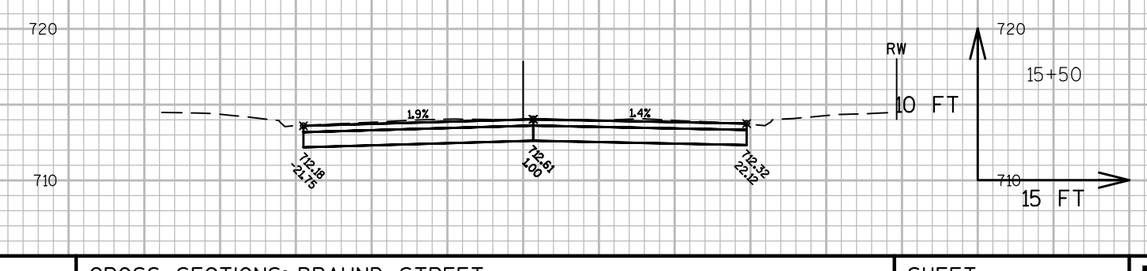












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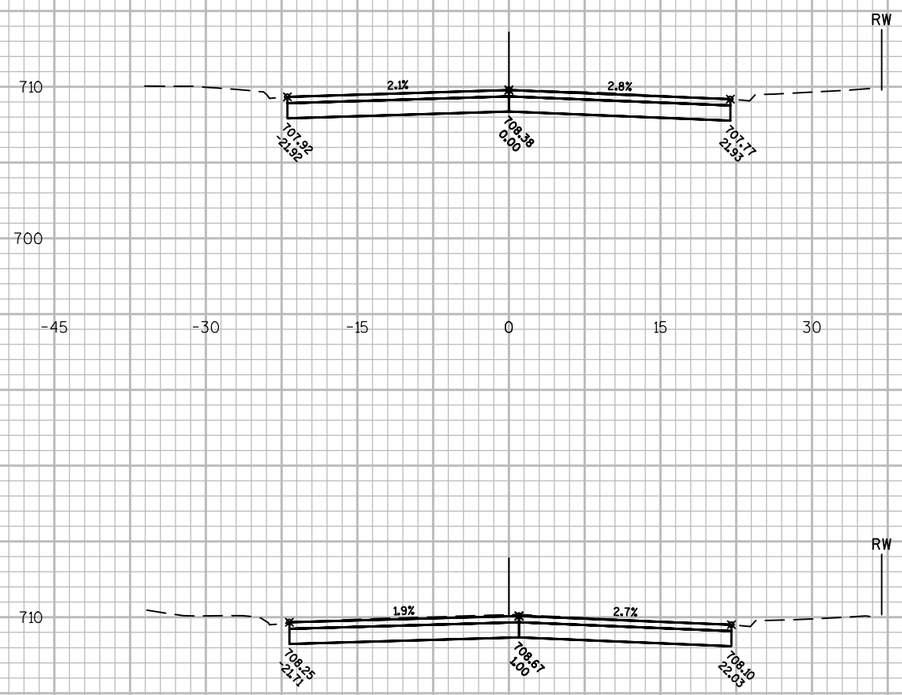
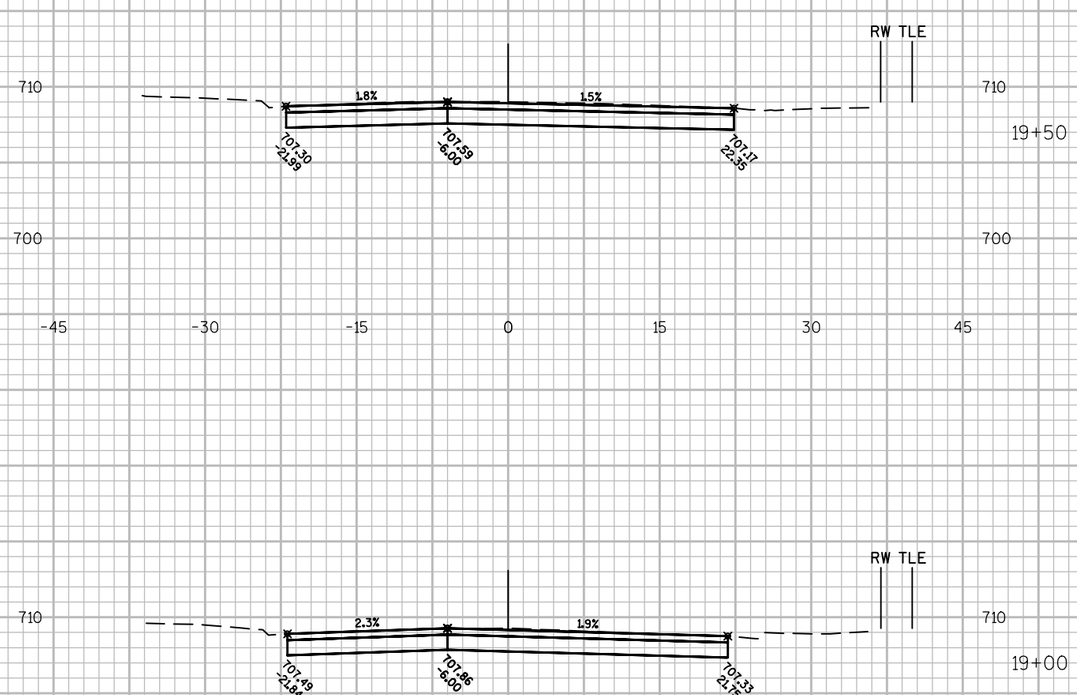
9

PROJECT NO: 5991-02-54	HWY: BRAUND STREET	COUNTY: LA CROSSE	CROSS SECTIONS: BRAUND STREET	SHEET 92	E

FILE NAME : \\SEHLX1\PROJECTS\KO\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-BRAUND.DWG
LAYOUT NAME - 090201-XS-BRAUND - 0902 - (2)
PLOT DATE : 1/3/2017 3:45 PM
PLOT BY : TOREY LEONARD
PLOT NAME :
PLOT SCALE : 1 IN:15 FT
WISDOT/CADDS SHEET 49



18+00



19+50

17+50

19+00

17+00

18+50

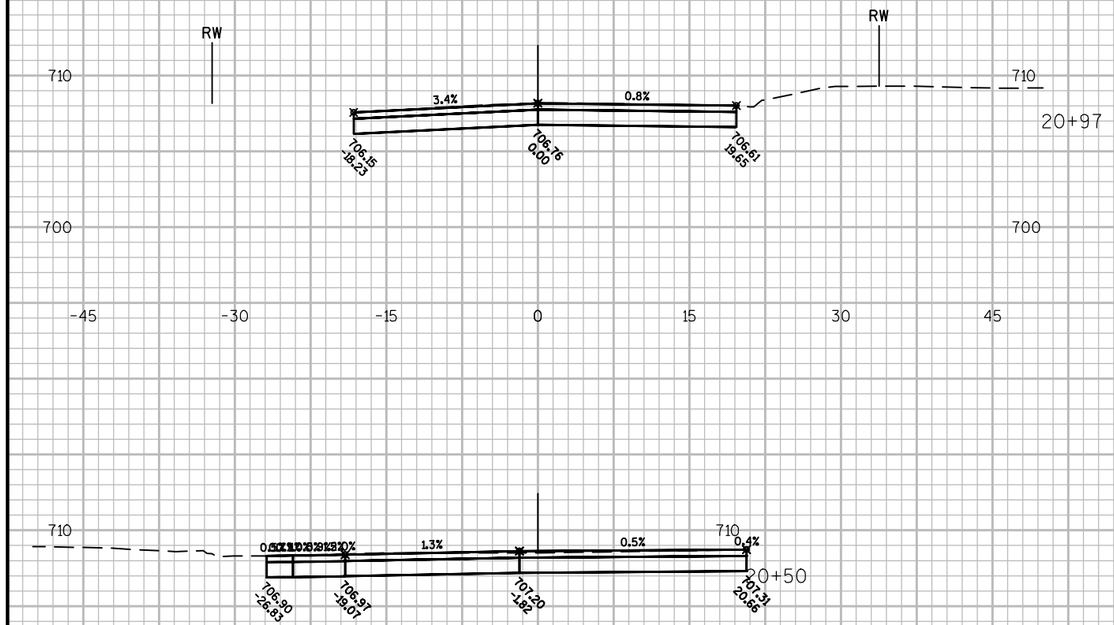
9

9

PROJECT NO: 5991-02-54	HWY: BRAUND STREET	COUNTY: LA CROSSE	CROSS SECTIONS: BRAUND STREET	SHEET 93	E

FILE NAME : \\SEHLX1\PROJECTS\K0\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-BRAUND.DWG
LAYOUT NAME - 090201-XS-BRAUND - 0902 - (3)

PLOT DATE : 1/3/2017 3:45 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:15 FT



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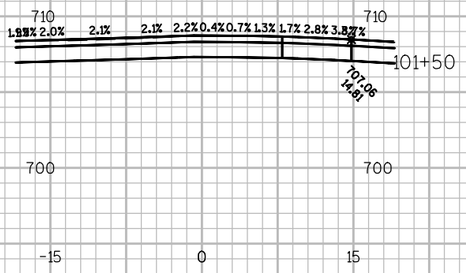
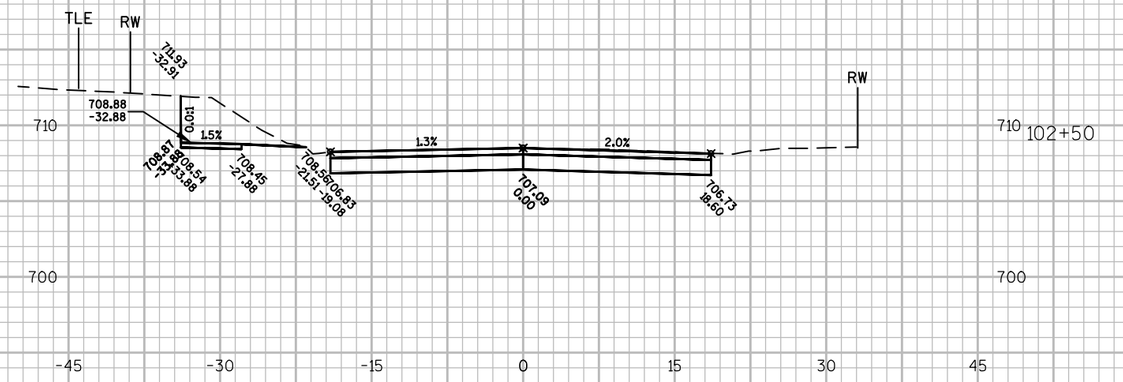
9

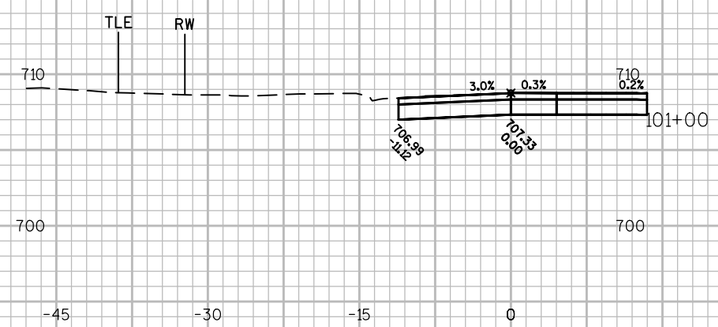
PROJECT NO: 5991-02-54	HWY: BRAUND STREET	COUNTY: LA CROSSE	CROSS SECTIONS: BRAUND ST	SHEET 94	E

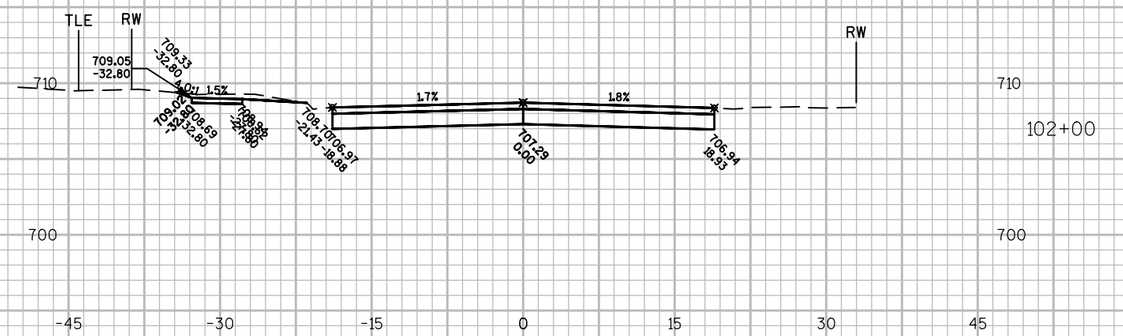
FILE NAME : \\SEHLX1\PROJECTS\K0\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-BRAUND.DWG
LAYOUT NAME - 090201-XS-BRAUND - 0902 - (4)

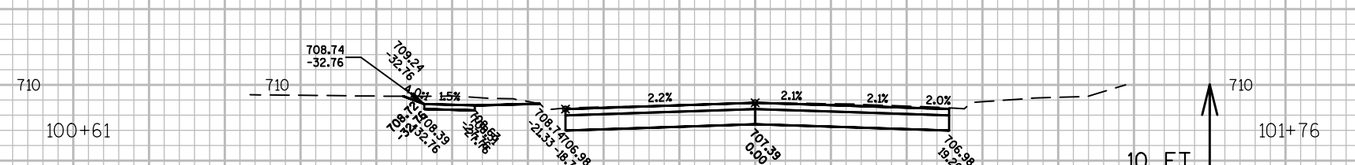
PLOT DATE : 1/3/2017 3:46 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:15 FT









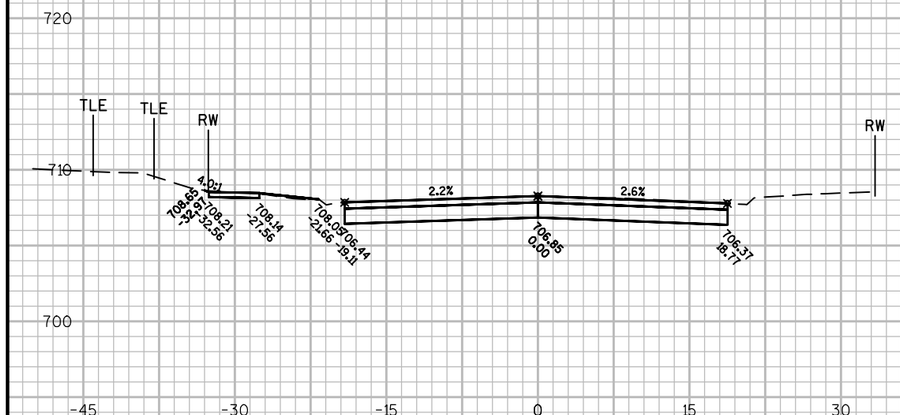


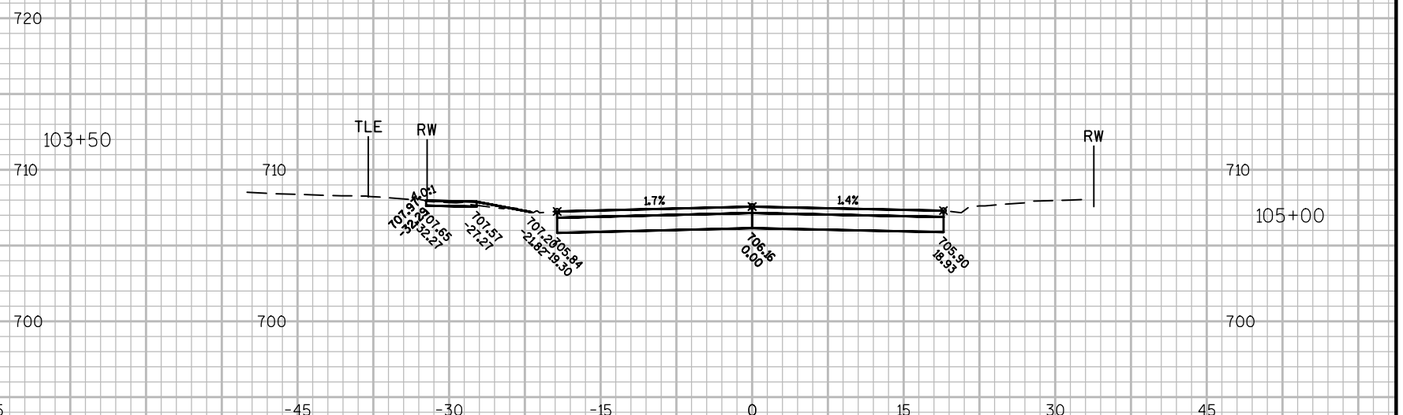
9

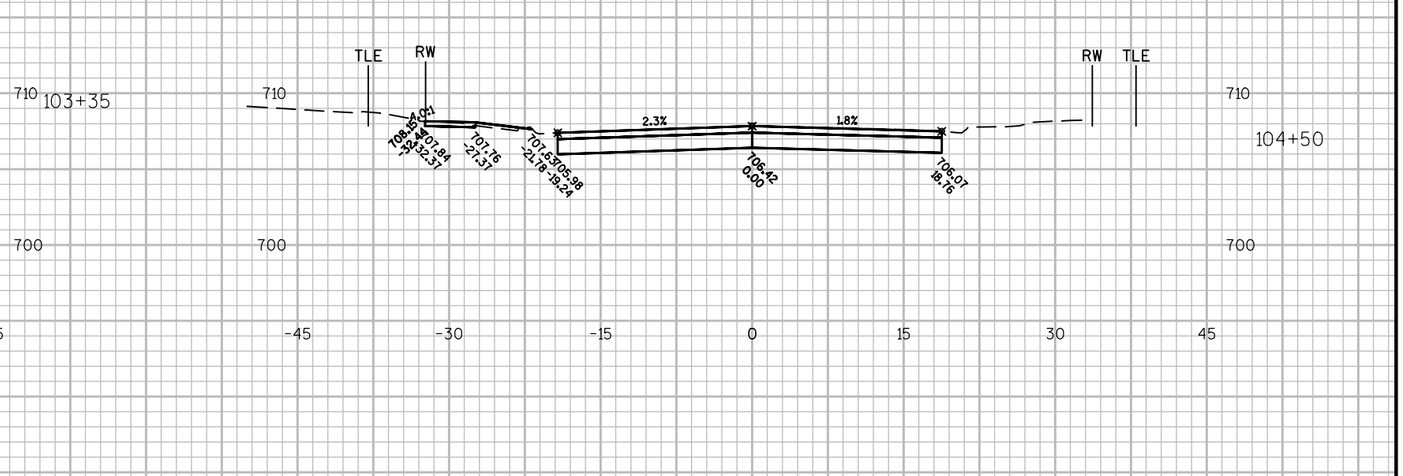
9

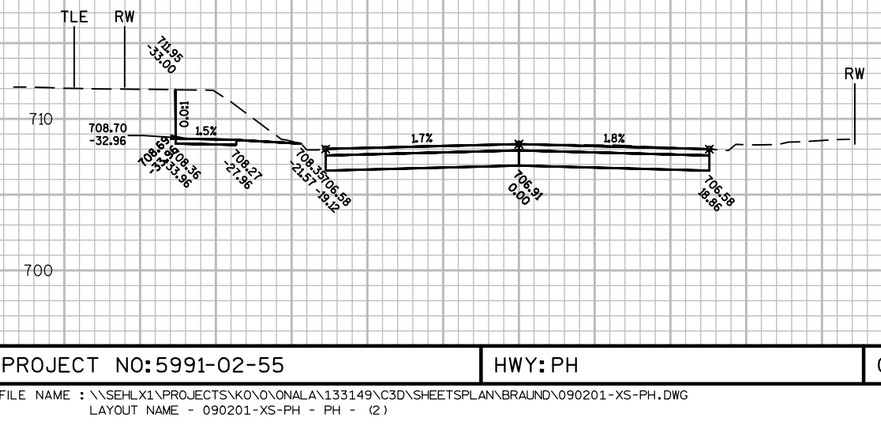
PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	CROSS SECTIONS: PH	SHEET 95	E

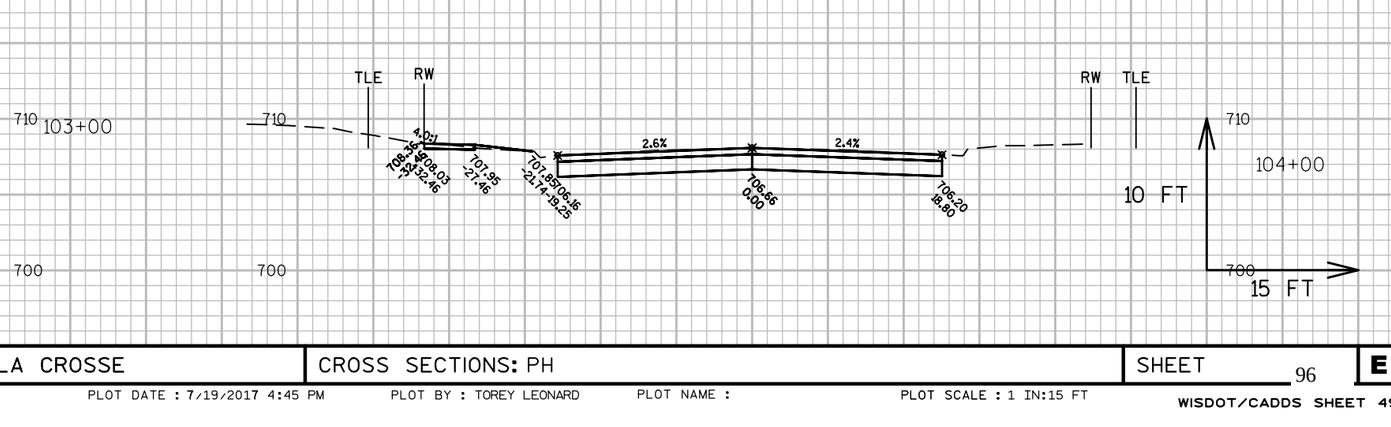
FILE NAME : \\SEHLX1\PROJECTS\KO\O\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-PH.DWG
LAYOUT NAME - 090201-XS-PH - PH - (1)
PLOT DATE : 7/19/2017 4:44 PM
PLOT BY : TOREY LEONARD
PLOT NAME :
PLOT SCALE : 1 IN:15 FT
WISDOT/CADDS SHEET 49











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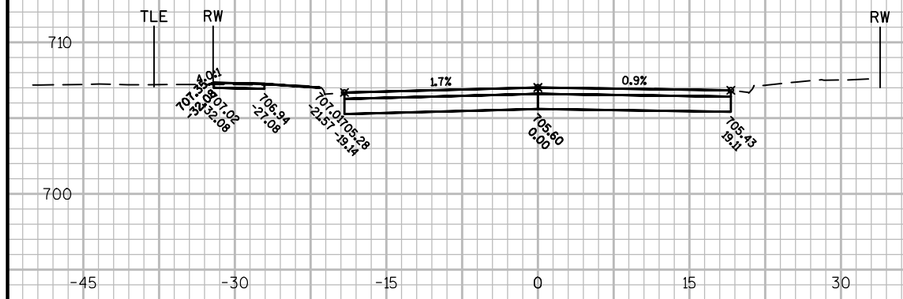
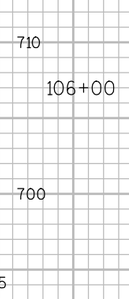
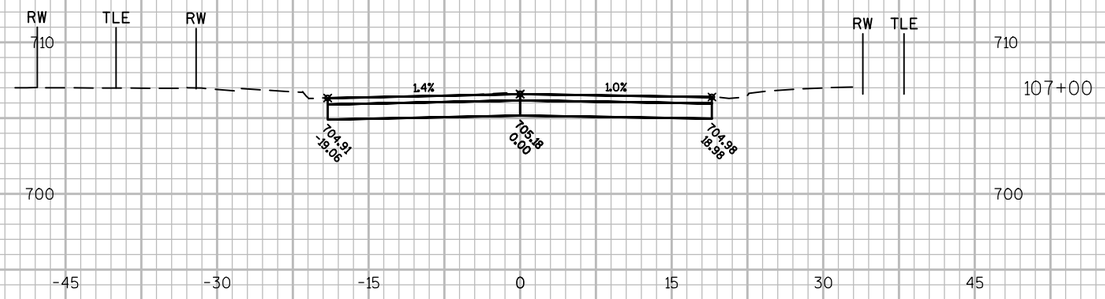
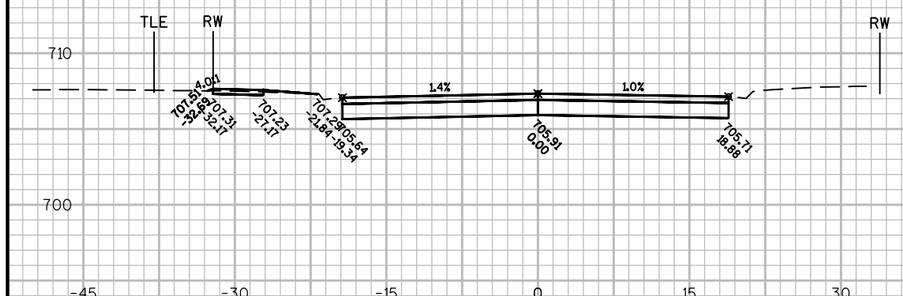
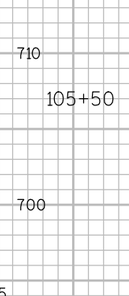
PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	CROSS SECTIONS: PH	SHEET 96	E

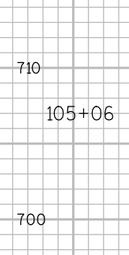


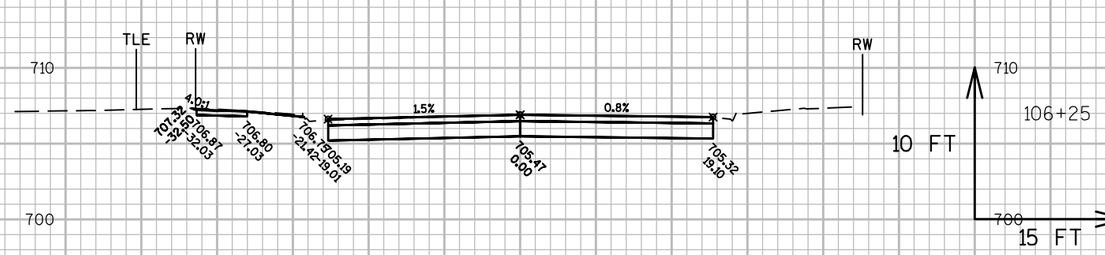












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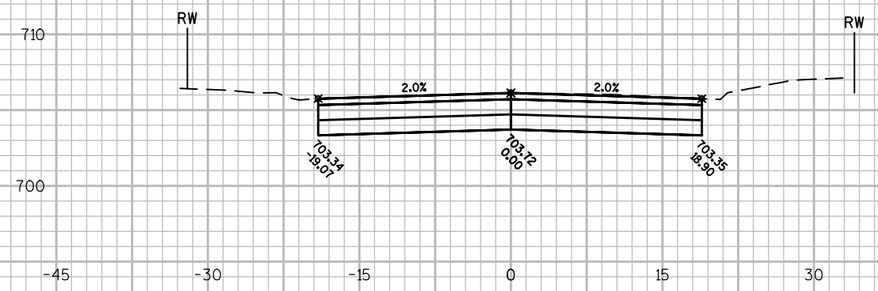
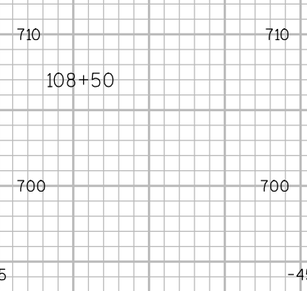
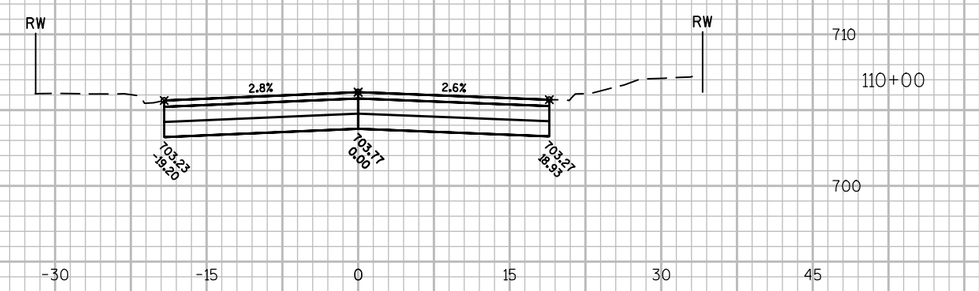
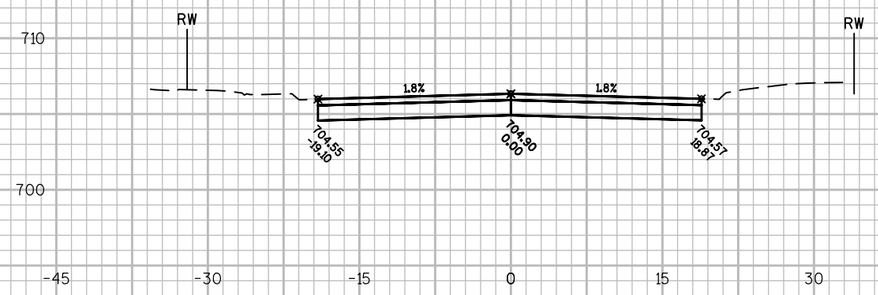
PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	CROSS SECTIONS: PH	SHEET 97	E

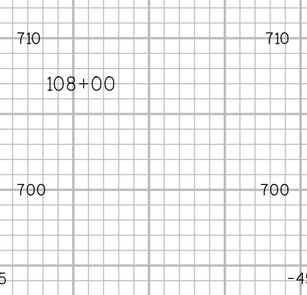
FILE NAME : \\SEHLX1\PROJECTS\KO\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-PH.DWG
LAYOUT NAME - 090201-XS-PH - PH - (3)
PLOT DATE : 7/19/2017 4:45 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:15 FT
WISDOT/CADDS SHEET 49

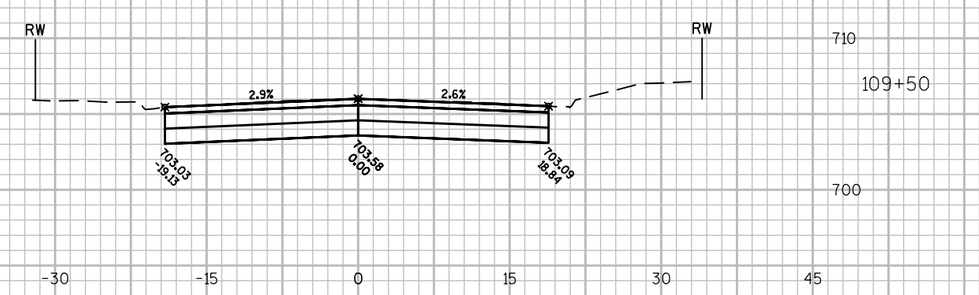


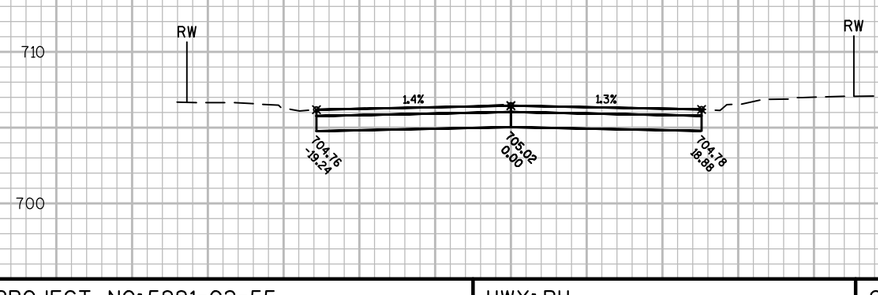


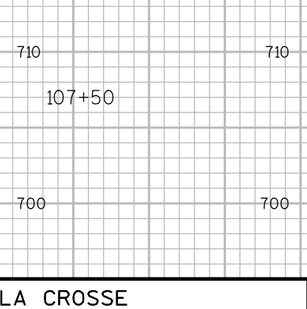


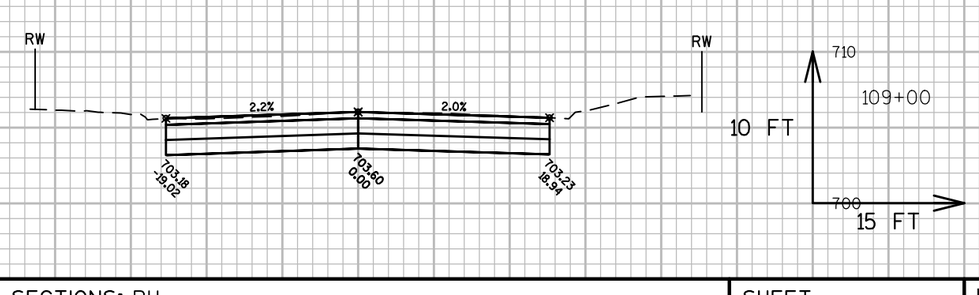












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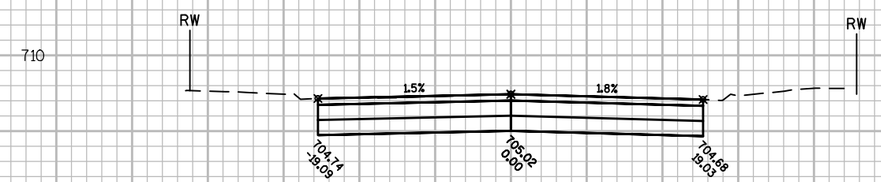
PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	CROSS SECTIONS: PH	SHEET 98	E

FILE NAME : \\SEHLX1\PROJECTS\KO\O\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-PH.DWG
LAYOUT NAME - 090201-XS-PH - PH - (4)

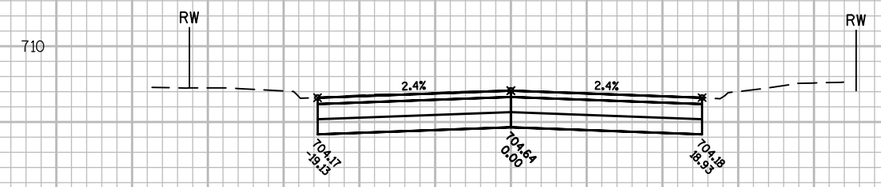
PLOT DATE : 7/19/2017 4:45 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:15 FT

WISDOT/CADDS SHEET 49



111+50

112+78



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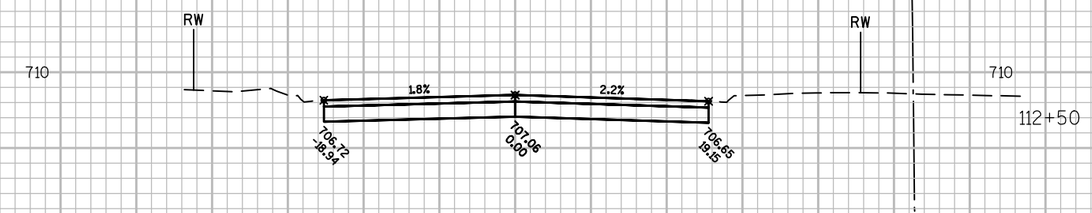

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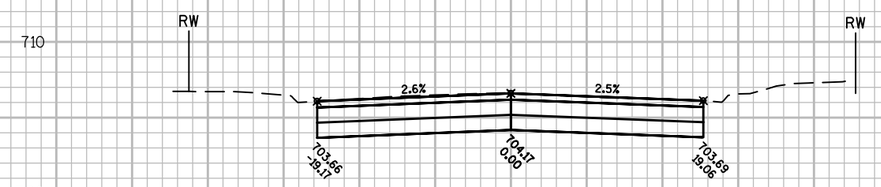

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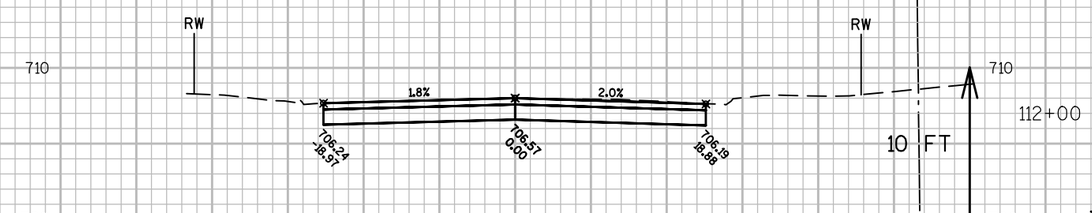

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PROJECT NO: 5991-02-55	HWY: PH	COUNTY: LA CROSSE	CROSS SECTIONS: PH	SHEET 99	E

FILE NAME : \\SEHLX1\PROJECTS\K0\0\ONALA\133149\C3D\SHEETSPLAN\BRAUND\090201-XS-PH.DWG
LAYOUT NAME - 090201-XS-PH - PH - (5)

PLOT DATE : 7/19/2017 4:45 PM PLOT BY : TOREY LEONARD PLOT NAME : PLOT SCALE : 1 IN:15 FT

Wisconsin Department of Transportation

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