

DNR/DOT PROJECT REVIEW

State of Wisconsin – Department of Natural Resources (DNR) and Department of Transportation (WisDOT)
DTNR0002 6/2018

DNR Internet: http://dnr.wi.gov/	WisDOT Internet: http://wisconsindot.gov/
ANDY BARTA DNR SOUTH CENTRAL REGION HEADQUARTERS 3911 FISH HATCHERY ROAD FITCHBURG, WI 53711	Wisconsin Department of Transportation Division of Transportation Systems Development 3550 MORMON COULEE ROAD LA CROSSE, WI 54601

Inform WisDOT Regional Environmental Coordinator, if more than 45 days is needed.

Design Project ID 5680-04-30	Project Highway STH 60	Review Submittal Date (m/d/yy) 7/26/19
Construction Project ID 5680-04-60	Estimated Project Cost (range) \$4,000,000.00 to \$4,300,000.00	Construction Year (yyyy) 2020
Project Name Spring Green - Sauk City	Project Limits USH 14 to Fox Road	
County Sauk	Project on Tribal Land <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Contact Name Nathaniel Schumaker	Contact (Area Code) Phone Number (608) 386-1334	
Section/Township/Range S02, T08N, R04E	Estimated Area of Ground Disturbance (acres) 1.02 acres	

Type of Review Requested <input type="checkbox"/> Initial Review <input checked="" type="checkbox"/> Final Concurrence <input type="checkbox"/> Scope Change <input type="checkbox"/> Other:	Document Type <input type="checkbox"/> Environmental Assessment (EA) <input type="checkbox"/> Environmental Report (ER) <input type="checkbox"/> Programmatic Categorical Exclusion (PCE) <input checked="" type="checkbox"/> Categorical Exclusion Checklist (CEC)
WisDOT Project Classification <input type="checkbox"/> Bridge Rehabilitation, FDM 3-5-2 <input type="checkbox"/> Bridge Replacement, FDM 3-5-2 <input type="checkbox"/> Expansion, FDM 3-5-2 <input type="checkbox"/> Pavement Replacement, FDM 3-5-2 <input type="checkbox"/> Preventive Maintenance, FDM 3-1-5 <input type="checkbox"/> SHRM (State Hwy Rehab/Maint), Maintenance Manual 13.08 <input checked="" type="checkbox"/> Recondition, FDM 3-5-2 <input type="checkbox"/> Reconstruction, FDM 3-5-2 <input type="checkbox"/> Resurface, FDM 3-5-2 <input type="checkbox"/> Safety (HSIP), PMM 4-1-10 <input type="checkbox"/> Other:	Work Involved <input checked="" type="checkbox"/> Beam Guard Replacement <input checked="" type="checkbox"/> Borrow and/or Waste Site Required <input type="checkbox"/> Channel Change/Stream Relocation <input type="checkbox"/> Clearing and Grubbing <input type="checkbox"/> Culvert Replacement or Extensions <input type="checkbox"/> Dredging <input type="checkbox"/> Grading <input checked="" type="checkbox"/> Fill Outside Toe of Slope <input type="checkbox"/> Intersection Improvement <input type="checkbox"/> Right of Way Acquisition <input checked="" type="checkbox"/> Shoulder Work <input type="checkbox"/> Storm Sewer <input type="checkbox"/> Other:

Storm Water Management (check all that apply)

- ☐ Trans 401 post construction requirements
☐ NPDES MS4/Urbanized Area
☐ TMDL Implementation Area
☒ Transportation Construction General Permit (TCGP)

For more information, please see the back of this form.

Project Description and Reason for Project:

(include project location map with limits and necessary attachments; attach additional sheets if needed)

The purpose of this project is to preserve and improve the existing pavement structure with a 5" mill and relay of the existing asphaltic pavement and place a 3.5" HMA pavement over the recycled base. The proposed alternative will maintain the existing roadway width of 30-32 feet as shown in the attached typical sections. In addition to the pavement improvement, guardrail terminals are to be upgraded as needed; new installations will be installed. A relatively low quantity of borrow will be required for the necessary grading work for the new MGS energy absorbing terminals. There is no proposed culvert work at this time because that endeavour has been assigned to Sauk County maintenance personnel. There is no proposed structure work.

DNR Comments from initial review letter:

- 1) Section 4(f) Requirement - This project proposes no right-of-way easements or acquisitions. No impacts are anticipated to public lands.
- 2) A preliminary Wetland Impact Tracking Form has been completed with a total of 0.07 acres of RPE & RPF wetland impacts.
- 3) Fisheries/Stream work - No in-water or culvert work is planned for this project, and no impacts to fisheries or aquatic systems are anticipated. The design team is working with the County Highway Department to ensure they have been addressing concerns from the WDNR's initial comments letter.
- 4) Endangered Resources -
 - Rusty Patched Bumble Bee - The department will use seed mix #70 in all disturbed areas.
 - Loggerhead Shrike - No clearing or grubbing is proposed. Should that change, the department will have county forces perform the work outside the avoidance period of 4/15 to 8/15.
 - Based on a field review by the WDNR in July of 2019, there were no signs of Narrow-Leaved Dayflower or Purple Milkweed.
- 5) Invasive species - The STP for aquatic exotic species control has been added to the special provisions.
- 6) The project area is adjacent to a mapped floodplain. There are no proposed changes to the waterway geometry, and there are minor changes to the horizontal geometry. From sta. 361 to sta. 377, the department is proposing to shift the alignment north by up to two feet to minimize fill impacts to the wetland along the south side of STH 60. The north side of this section is outside the mapped floodplain.
- 7) Storm Water & Erosion Control - The department will apply for and adhere to the TCGP.
- 8) Special Features - The department is proposing to use seeding mix #70A and Erosion Mat Urban Class I Type B.
- 9) Unofficial Boat Launch - Due to the limited right-of-way, steep slopes & sensitive wetland areas, the shoulder could not be widened and/or guard could not be relocated to maintain the pull-off for the boat launch. The department is not proposing to close or eliminate the access to the river, but there will no longer be a pad to park a vehicle on.
- 10) Other issues - The department will work with the region's REC to coordinate the 404 permit process.

Attachments:

- #1 - Project Location Map
- #2 - Typical Sections
- #3 - Plan & Construction Details
- #4 - 1957 Asbuilt
- #5 - Wetland Impact Tracking Form & Impacted Wetland Details
- #6 - FEMA Floodplain Maps (Firmettes)
- #7 - WDNR Initial Comments Letter

Attachment 1: Project Location Map

Wisconsin Department of Transportation

State Project I.D. 5680-04-30/60

Spring Green – Sauk City

USH 14 to Fox Road

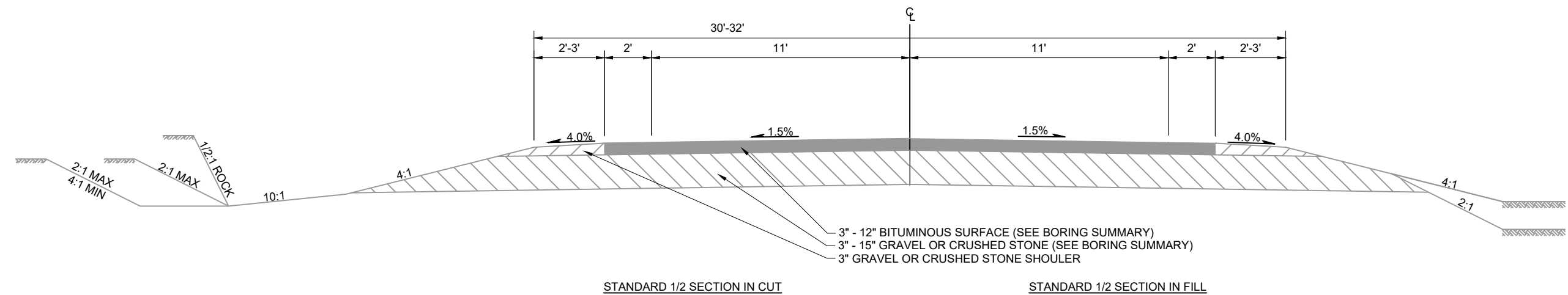
STH 60, Sauk County



Attachment 2: Typical Sections

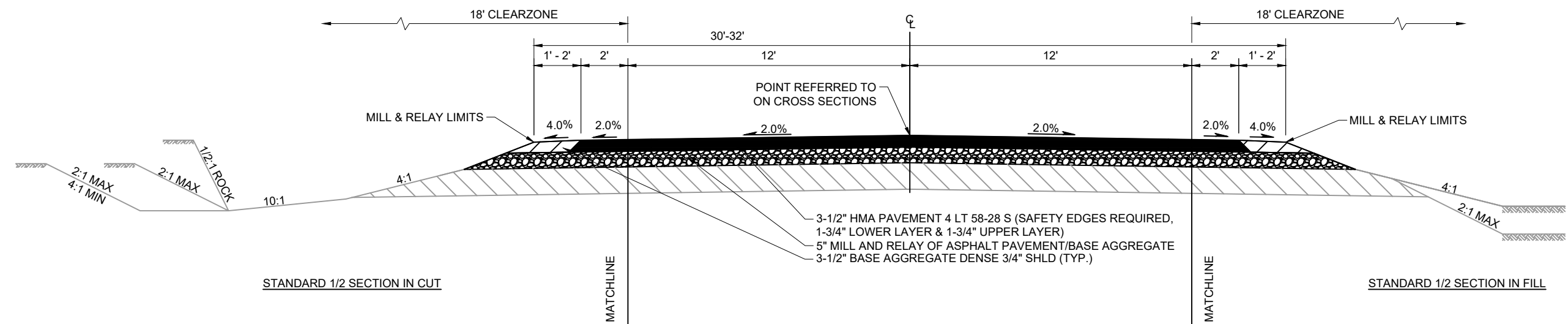
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2



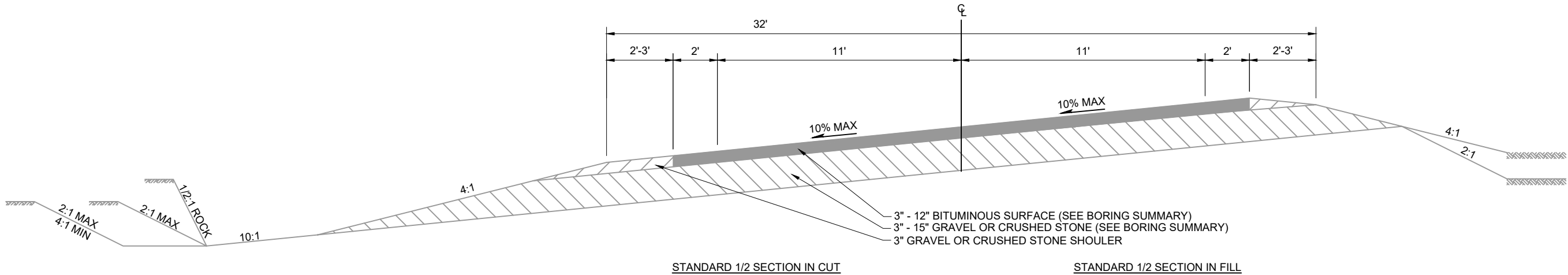
STH 60 EXISTING TYPICAL SECTION

STA. 0+93 TO STA. 205+00
STA. 218+00 TO STA. 314+00
STA. 416+00 TO STA. 549+63



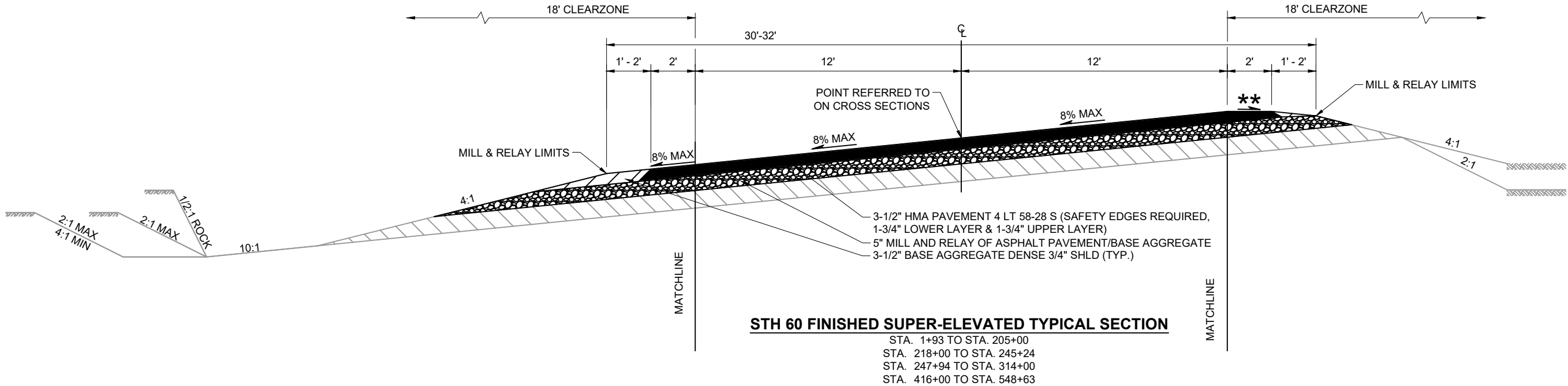
STH 60 FINISHED TYPICAL SECTION

STA. 1+93 TO STA. 205+00
STA. 218+00 TO STA. 245+24
STA. 247+94 TO STA. 314+00
STA. 416+00 TO STA. 548+63

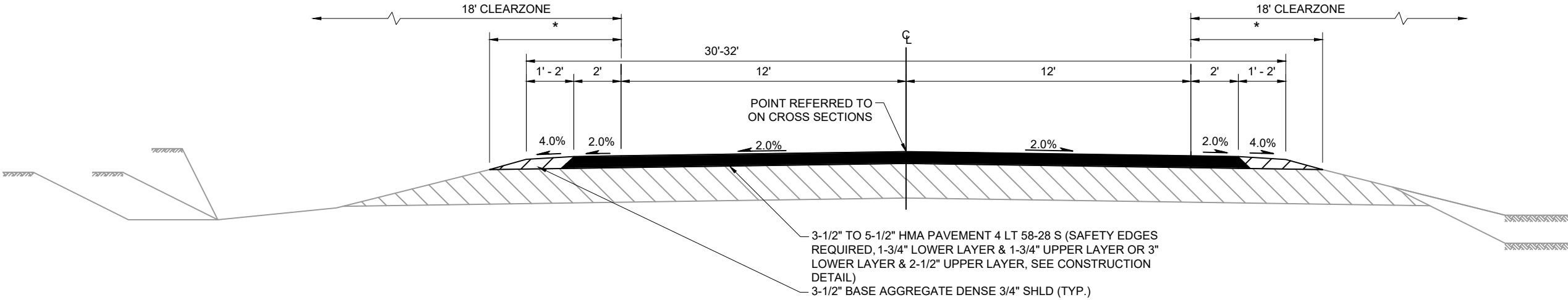


STH 60 EXISTING SUPER-ELEVATED TYPICAL SECTION

STA. 0+93 TO STA. 205+00
STA. 218+00 TO STA. 314+00
STA. 416+00 TO STA. 549+63

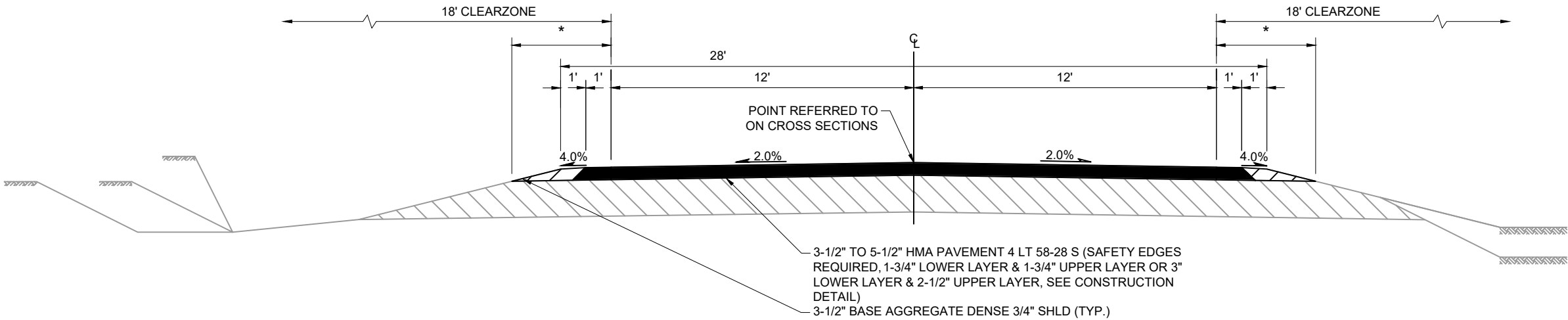


** 8% MAXIMUM ROLL-OVER RATE



STH 60 FINISHED TYPICAL SECTION

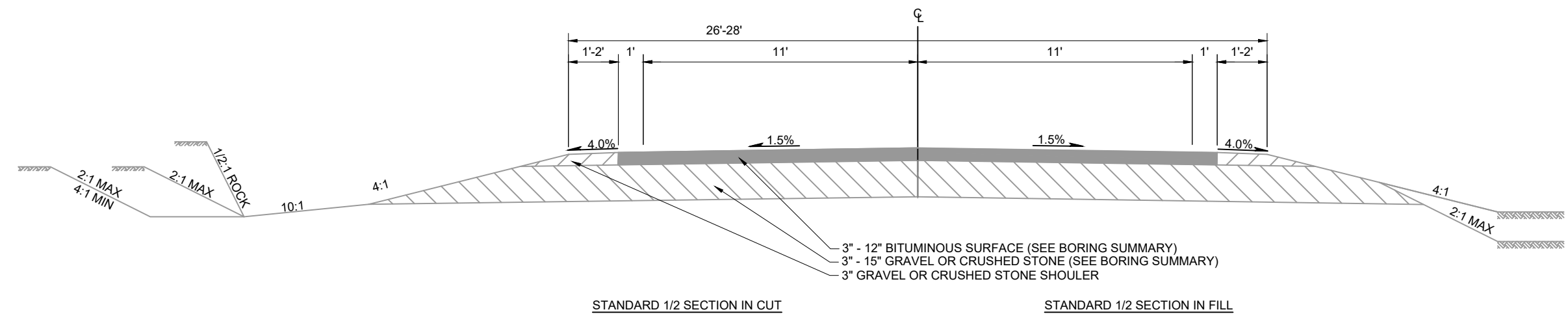
STA. 0+93 TO STA. 1+93
STA. 245+24 TO STA. 246+24
STA. 246+94 TO STA. 247+94
STA. 548+63 TO STA. 549+63



STH 60 FINISHED TYPICAL SECTION

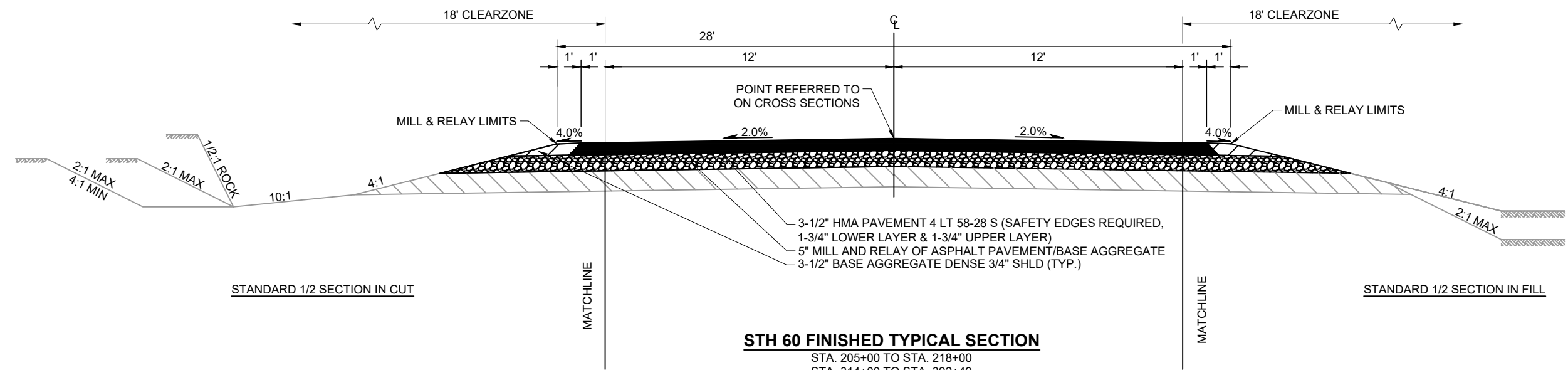
STA. 392+49 TO STA. 393+49
STA. 394+42 TO STA. 395+42

* LIMITS OF PREPARE FOUNDATION
FOR ASPHALTIC SHOULDERS



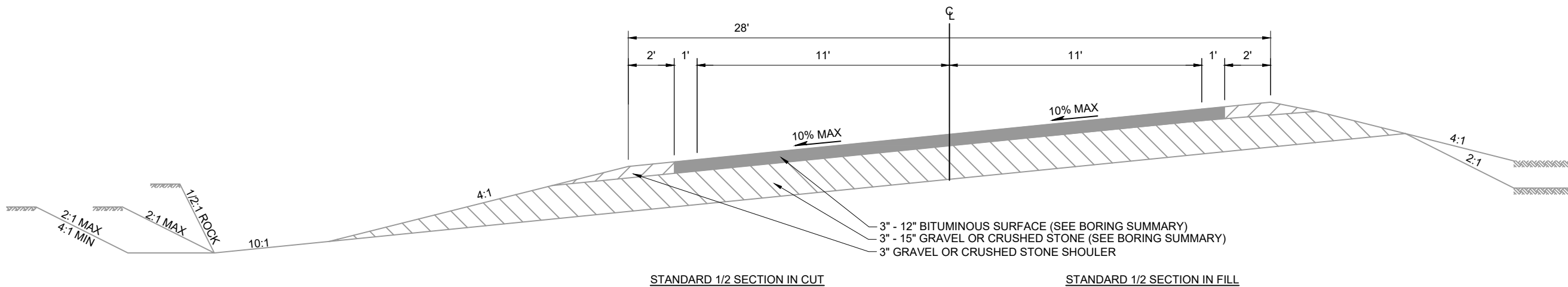
STH 60 EXISTING TYPICAL SECTION

STA. 205+00 TO STA. 218+00
STA. 314+00 TO STA. 416+00



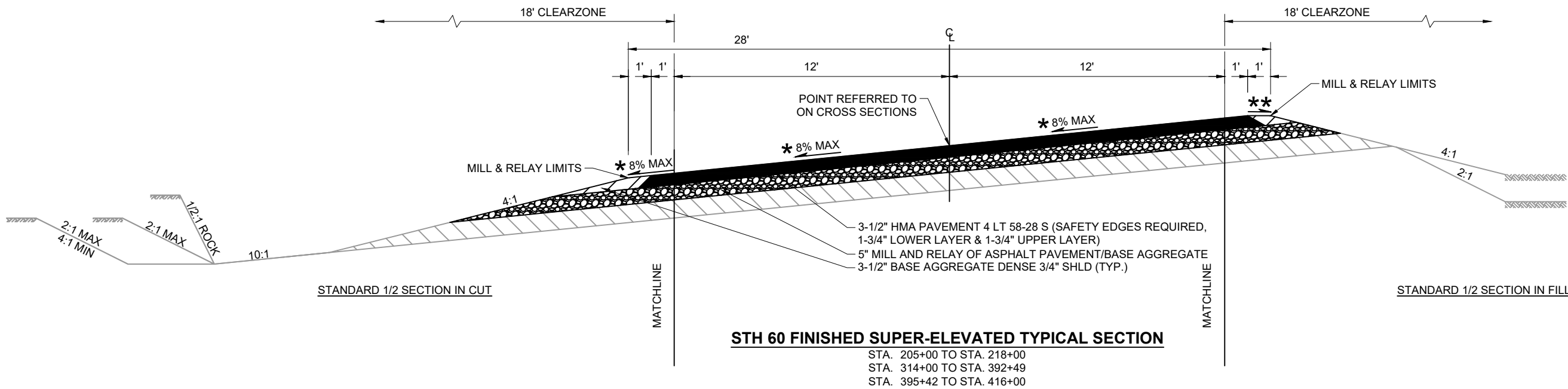
STH 60 FINISHED TYPICAL SECTION

STA. 205+00 TO STA. 218+00
STA. 314+00 TO STA. 392+49
STA. 395+42 TO STA. 416+00



STH 60 EXISTING SUPER-ELEVATED TYPICAL SECTION

STA. 205+00 TO STA. 218+00
STA. 314+00 TO STA. 416+00



STH 60 FINISHED SUPER-ELEVATED TYPICAL SECTION

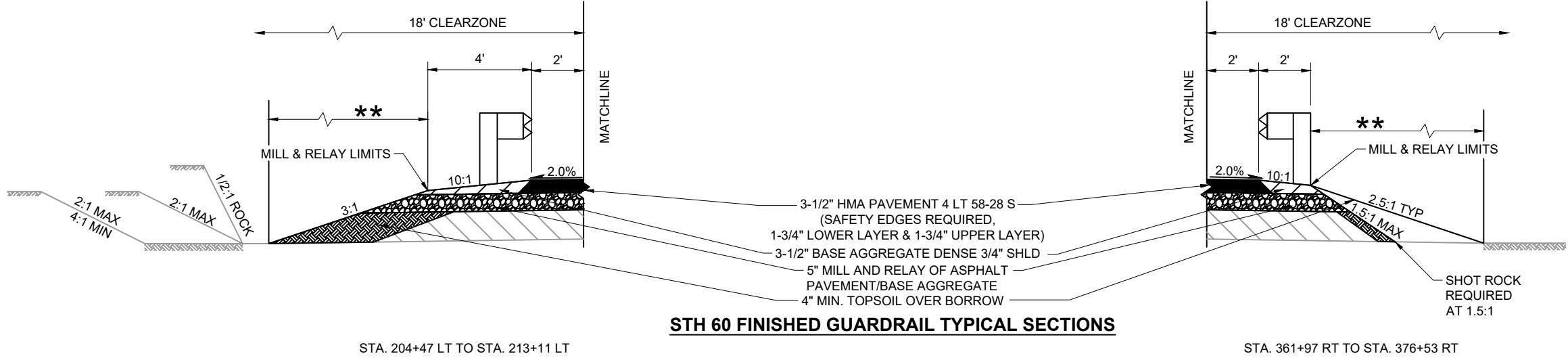
STA. 205+00 TO STA. 218+00
STA. 314+00 TO STA. 392+49
STA. 395+42 TO STA. 416+00

* 10% SUPER-ELEVATION FROM
STA. 392+43 TO STA. 395+989 TO
MATCH B-56-0022.

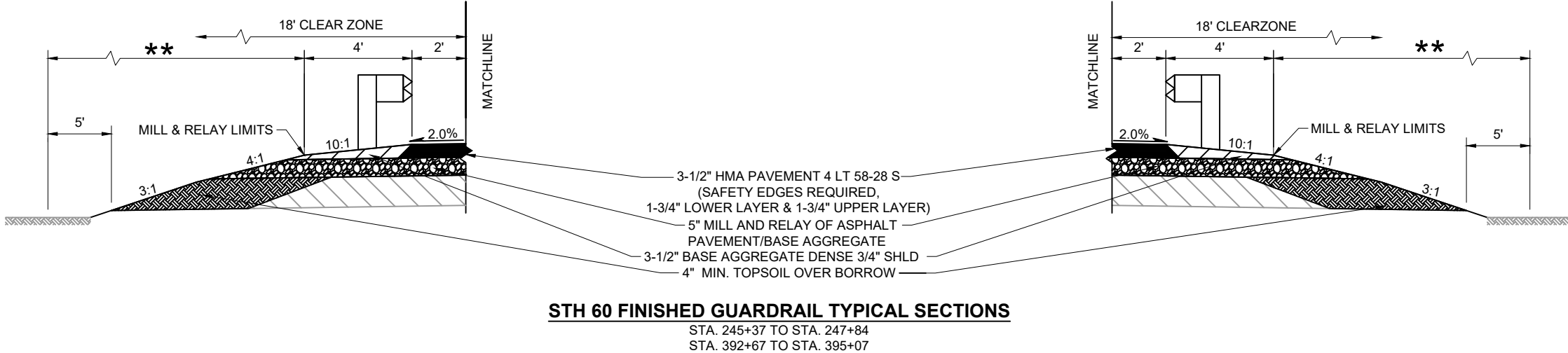
** 8% MAXIMUM ROLL-OVER RATE

Attachment 2: Typical Sections

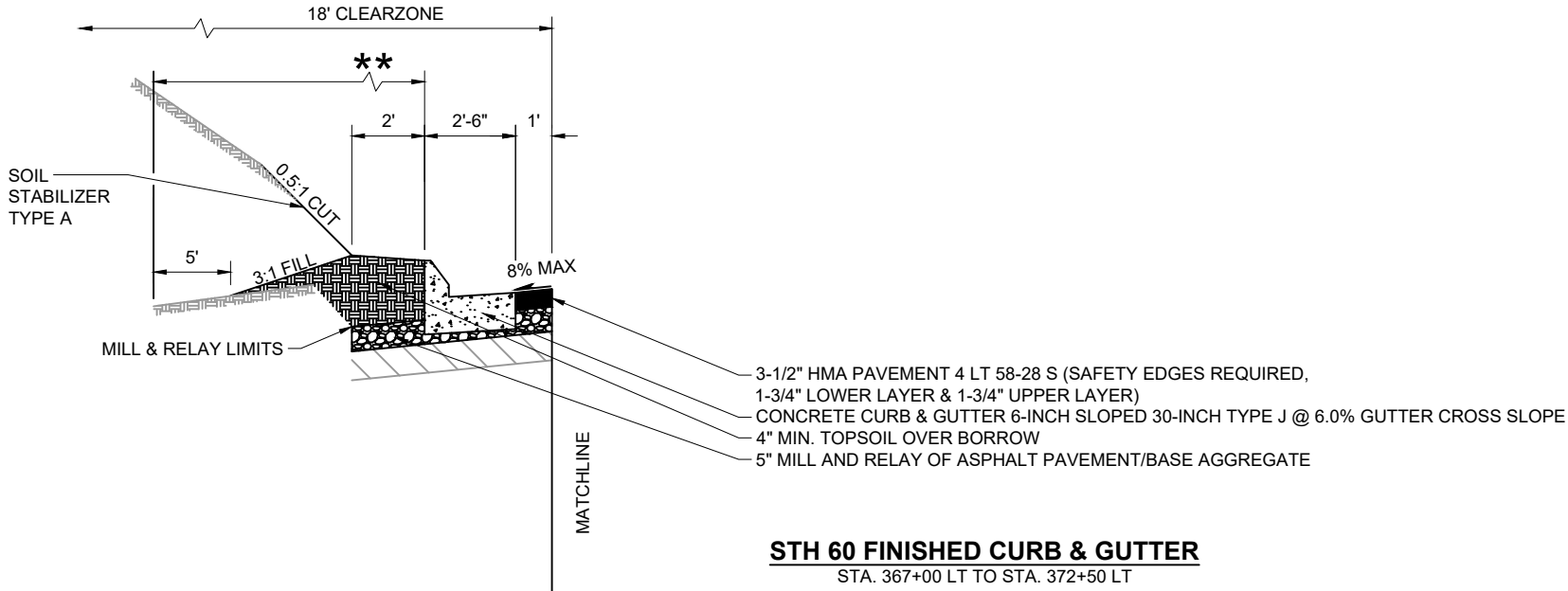
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STH 60 FINISHED GUARDRAIL TYPICAL SECTIONS



STH 60 FINISHED GUARDRAIL TYPICAL SECTIONS



STH 60 FINISHED CURB & GUTTER

** LIMITS OF SEEDING, FERTILIZER, AND EMAT END 5 FEET BEYOND TOE OF SLOPE EXCEPT WHEN ADJACENT TO WATER.

PROJECT NO: 5680-04-60

HWY: STH 60

COUNTY: SAUK

TYPICAL SECTIONS

SHEET

E

FILE NAME : N:\PDS\C3D\56800430\SHEETSP\020301_TS.DWG
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PLOT NAME :

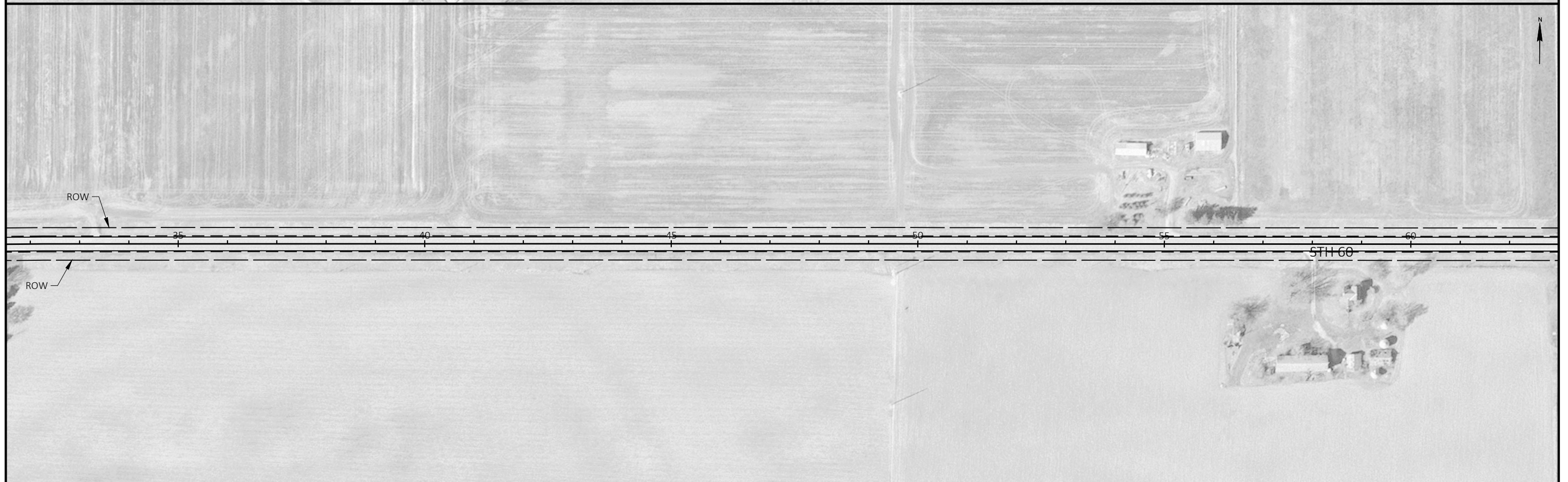
PLOT SCALE : 1 IN:5 FT

WISDOT/CADDs SHEET 42

Attachment 3: Plan & Construction Details

2

2



PROJECT NO:	5680-04-60
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HWY: STH 60

COUNTY: SAUK

PLAN DETAILS

SHEET

I

FILE NAME : N:\PDS\C3D\56800430\SHEETSPLAN\021201_PD.DWG
LAYOUT NAME - 01

PLOT DATE : 7/26/2019 8:06 AM

PLOT BY : SCHUMAKER, NATHANIEL

PLOT NAME :

PLOT SCALE : 1 IN:200 FT

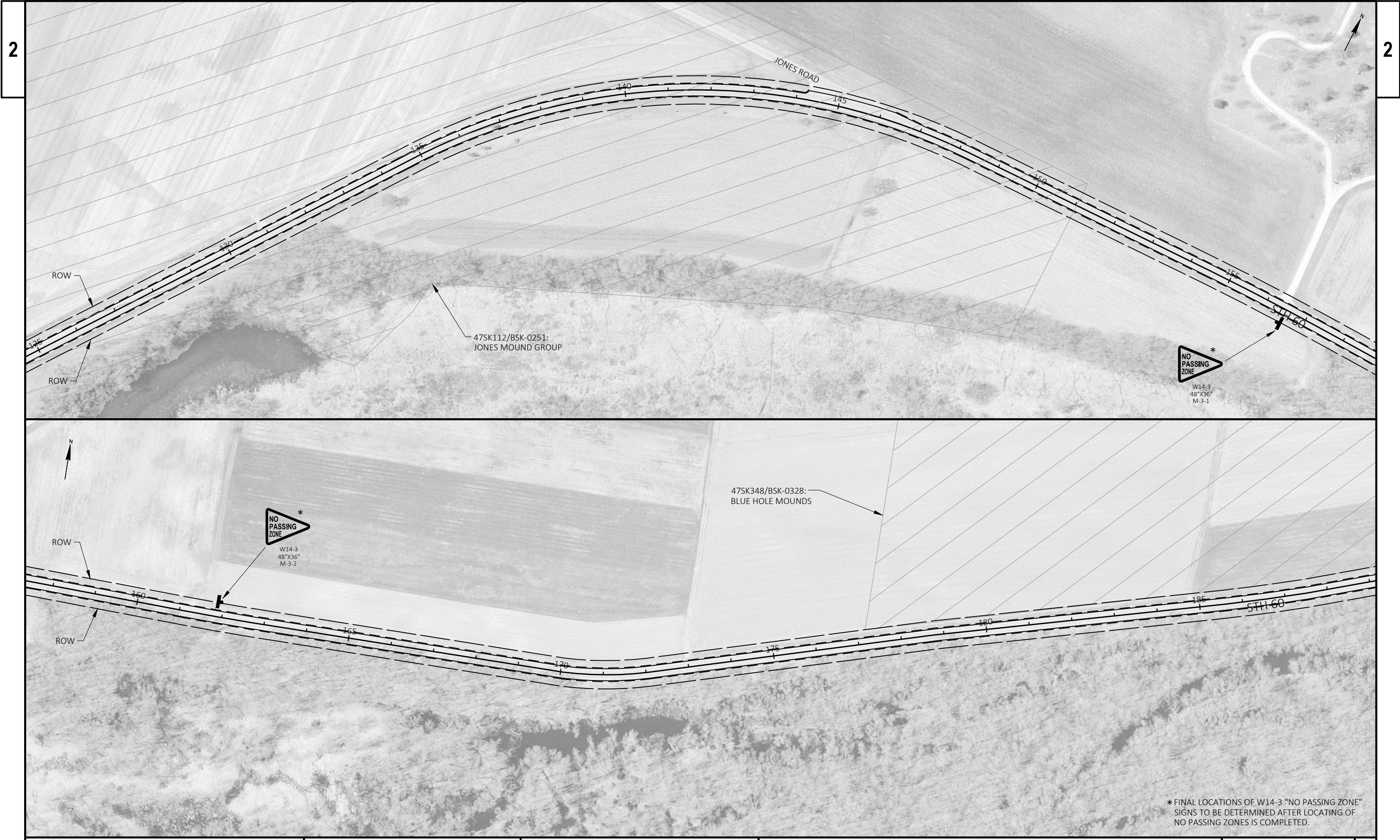
WISDOT/CADDS SHEET 44

Attachment 3: Plan & Construction Details



PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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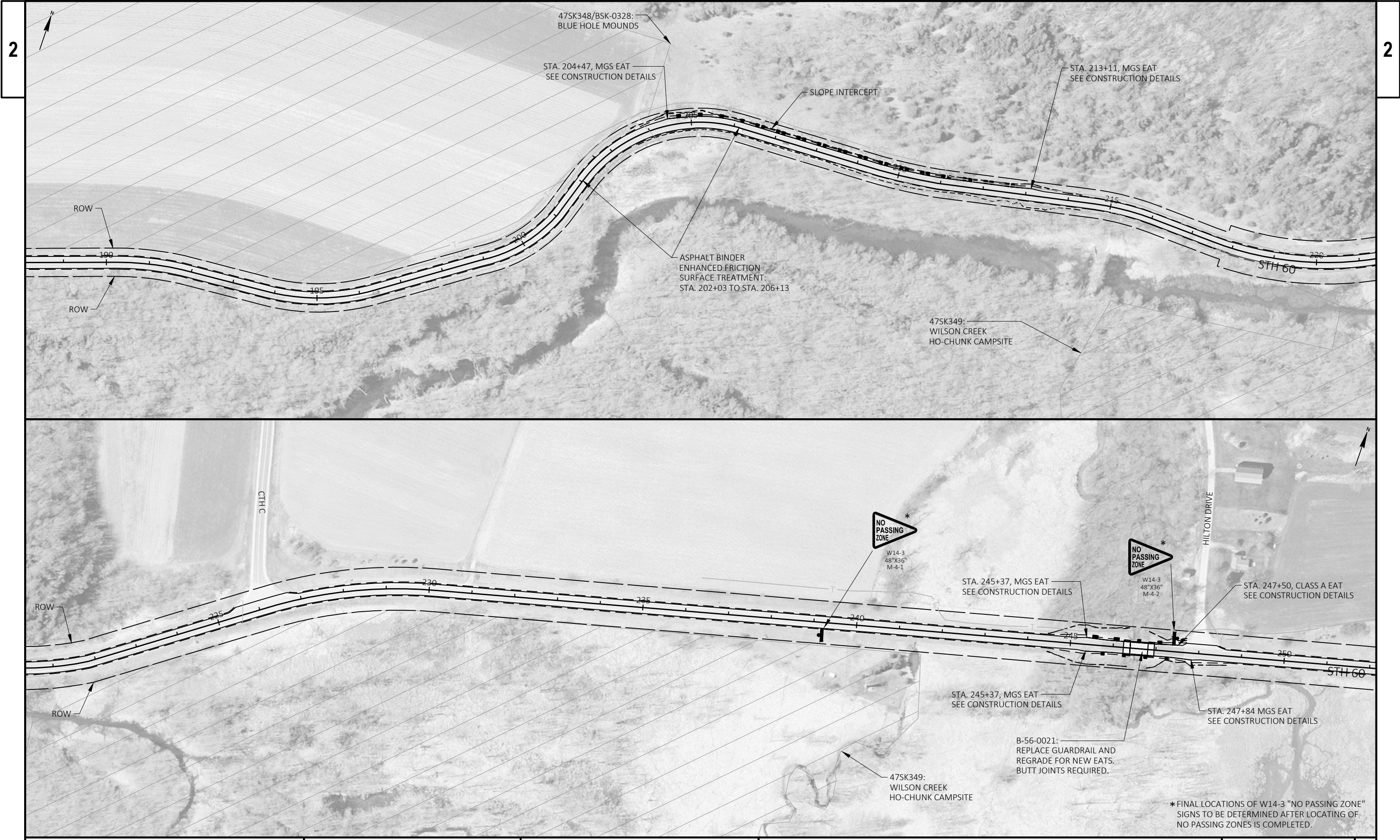
Attachment 3: Plan & Construction Details



* FINAL LOCATIONS OF W14-3 "NO PASSING ZONE" SIGNS TO BE DETERMINED AFTER LOCATING OF NO PASSING ZONES IS COMPLETED.

PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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Attachment 3: Plan & Construction Details



PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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Attachment 3: Plan & Construction Details



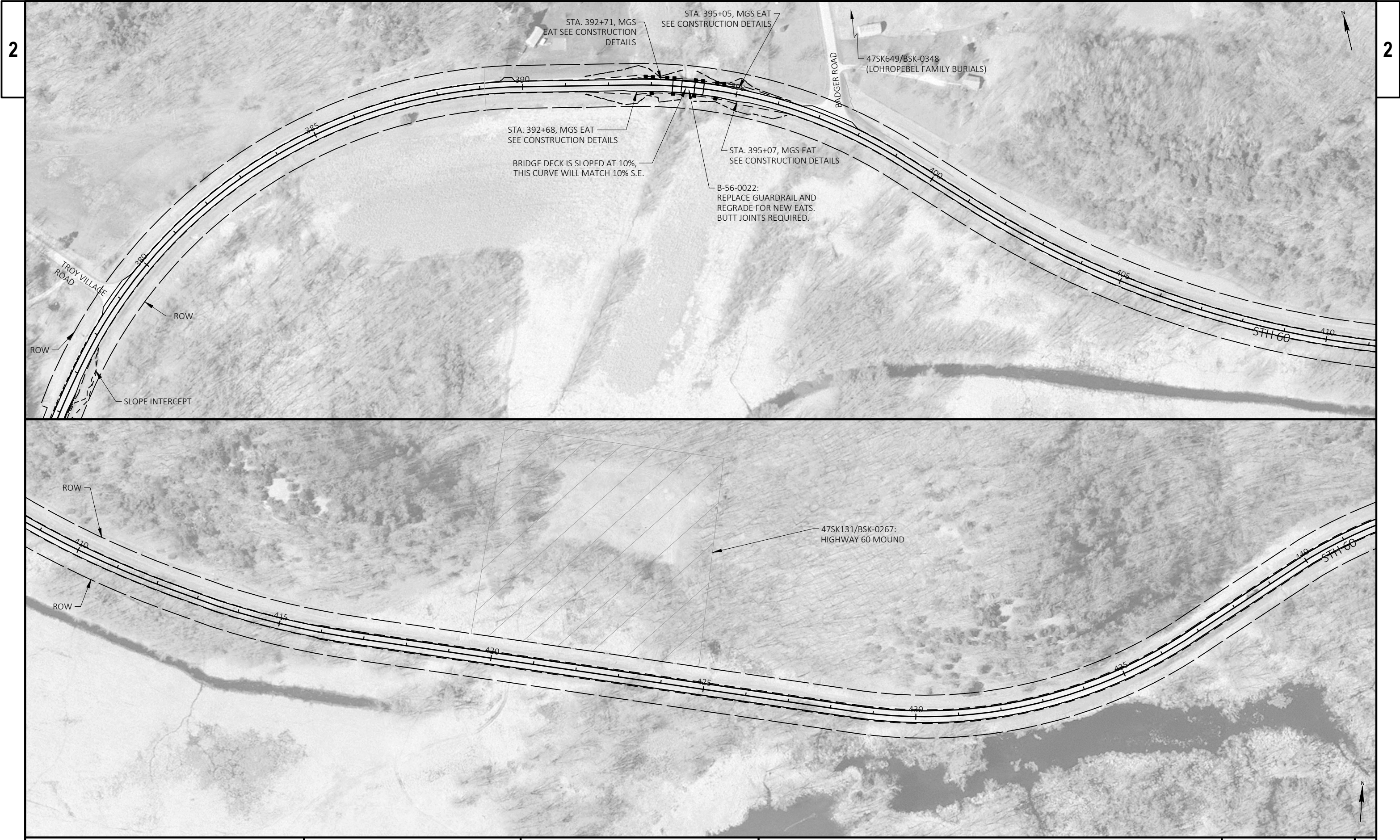
PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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Attachment 3: Plan & Construction Details



PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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Attachment 3: Plan & Construction Details



PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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Attachment 3: Plan & Construction Details



PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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Attachment 3: Plan & Construction Details



PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	PLAN DETAILS	SHEET	E
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[illegible]

This plan view illustrates a road project on STH 60, spanning stationing from 245 to 363. The drawing is divided into three sections, each with specific stationing ranges and detailed engineering callouts.

Section 1 (Stationing 245-248):

- Left Side (Stationing 245-246):**
 - EDGE OF ASPHALT TAPER, STA. 245+17, 14.0' LT
 - EDGE OF ASPHALT TAPER, POST #1 MGS EAT: STA. 245+37, 16.0' LT
 - EDGE OF ASPHALT TAPER, POST #9 MGS EAT: STA. 245+87, 14.0' LT
 - START OF MGS 3: STA. 245+90, 14.0' LT
 - END MGS 3, BEGIN MGS THRIE BEAM TRANSITION: STA. 246+03, 14.0' LT
 - END MGS THRIE BEAM TRANSITION: STA. 246+42, 14.0' LT
 - BEGIN STEEL THRIE BEAM STRUCTURE APPROACH: STA. 246+79, 14.0' LT
- Right Side (Stationing 247-248):**
 - EDGE OF ASPHALT TAPER, END STEEL THRIE BEAM STRUCTURE APPROACH, POST #9 STEEL PLATE BEAM GUARD EAT: STA. 246+99, 14.0' LT
 - POST #1 STEEL PLATE BEAM GUARD EAT: STA. 247+49, 16.0' LT
 - EDGE OF ASPHALT TAPER, POST #1 MGS EAT: STA. 247+84, 16.0' RT
 - EDGE OF ASPHALT TAPER, POST #9 MGS EAT: STA. 247+34, 14.0' RT
 - EDGE OF ASPHALT TAPER: STA. 248+04, 14.0' RT

Section 2 (Stationing 248-250):

- Left Side (Stationing 248-249):**
 - EDGE OF ASPHALT TAPER, POST #9 MGS EAT: STA. 245+87, 14.0' RT
 - EDGE OF ASPHALT TAPER, POST #1 MGS EAT: STA. 245+37, 16.0' RT
 - EDGE OF ASPHALT TAPER: STA. 245+17, 14.0' RT
 - START OF MGS 3: STA. 245+90, 14.0' RT
 - END MGS 3, BEGIN MGS THRIE BEAM TRANSITION: STA. 246+03, 14.0' RT
 - END MGS THRIE BEAM TRANSITION: STA. 246+42, 14.0' RT
 - BEGIN MGS THRIE BEAM TRANSITION: STA. 246+79, 14.0' RT
 - END MGS THRIE BEAM TRANSITION, BEGIN MGS 3: STA. 247+19, 14.0' RT
 - END MGS 3: STA. 247+31, 14.0' RT
- Right Side (Stationing 249-250):**
 - EDGE OF ASPHALT TAPER, POST #1 MGS EAT: STA. 247+84, 16.0' RT
 - EDGE OF ASPHALT TAPER, POST #9 MGS EAT: STA. 247+34, 14.0' RT
 - EDGE OF ASPHALT TAPER: STA. 248+04, 14.0' RT

Section 3 (Stationing 250-363):

- Left Side (Stationing 250-251):**
 - EDGE OF ASPHALT TAPER, POST #9 MGS EAT: STA. 245+87, 14.0' RT
 - EDGE OF ASPHALT TAPER, POST #1 MGS EAT: STA. 245+37, 16.0' RT
 - EDGE OF ASPHALT TAPER: STA. 245+17, 14.0' RT
 - START OF MGS 3: STA. 245+90, 14.0' RT
 - END MGS 3, BEGIN MGS THRIE BEAM TRANSITION: STA. 246+03, 14.0' RT
 - END MGS THRIE BEAM TRANSITION: STA. 246+42, 14.0' RT
 - BEGIN MGS THRIE BEAM TRANSITION: STA. 246+79, 14.0' RT
 - END MGS THRIE BEAM TRANSITION, BEGIN MGS 3: STA. 247+19, 14.0' RT
 - END MGS 3: STA. 247+31, 14.0' RT
- Right Side (Stationing 251-252):**
 - EDGE OF ASPHALT TAPER, POST #1 MGS EAT: STA. 247+84, 16.0' RT
 - EDGE OF ASPHALT TAPER, POST #9 MGS EAT: STA. 247+34, 14.0' RT
 - EDGE OF ASPHALT TAPER: STA. 248+04, 14.0' RT

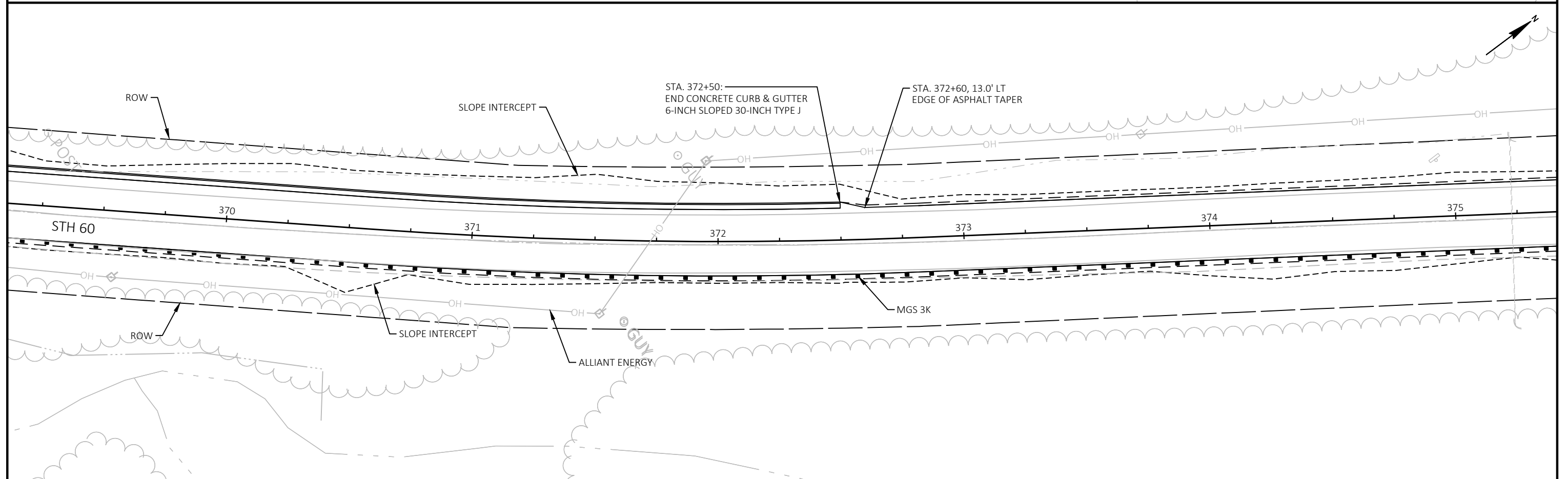
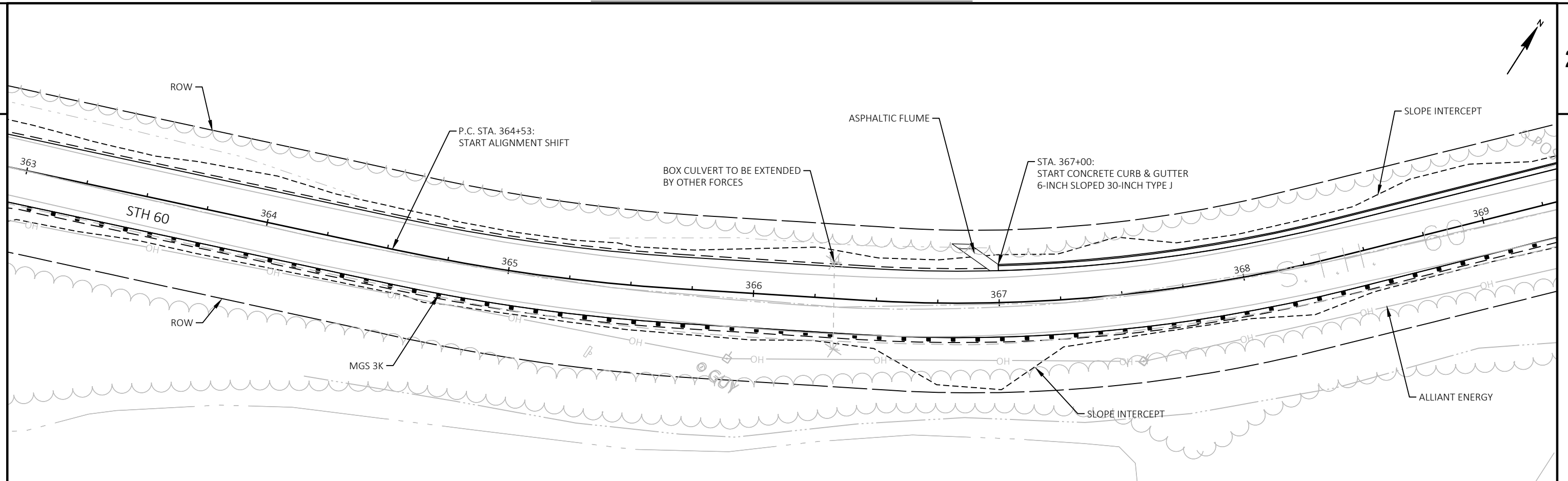
Other Features:

- ROW (Right of Way):** Indicated by dashed lines.
- SLOPE INTERCEPT:** Indicated by dashed lines.
- ALLIANT ENERGY:** Indicated by a dashed line.
- HILTON DRIVE:** Indicated by a dashed line.
- FRONTIER COMMUNICATIONS:** Indicated by a dashed line.
- STH 60:** The main road alignment.
- Stationing:** Marked along the top and bottom of the plan view.
- North Arrow:** Located in the top right corner.

Attachment 3: Plan & Construction Details

2

1



PROJECT NO:	5680-04-60
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HWY: STH 60

COUNTY: SAUK

CONSTRUCTION DETAIL

SHEET

1

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LAYOUT NAME - STA.362

PLOT DATE : 7/26/2019 8:15 AM

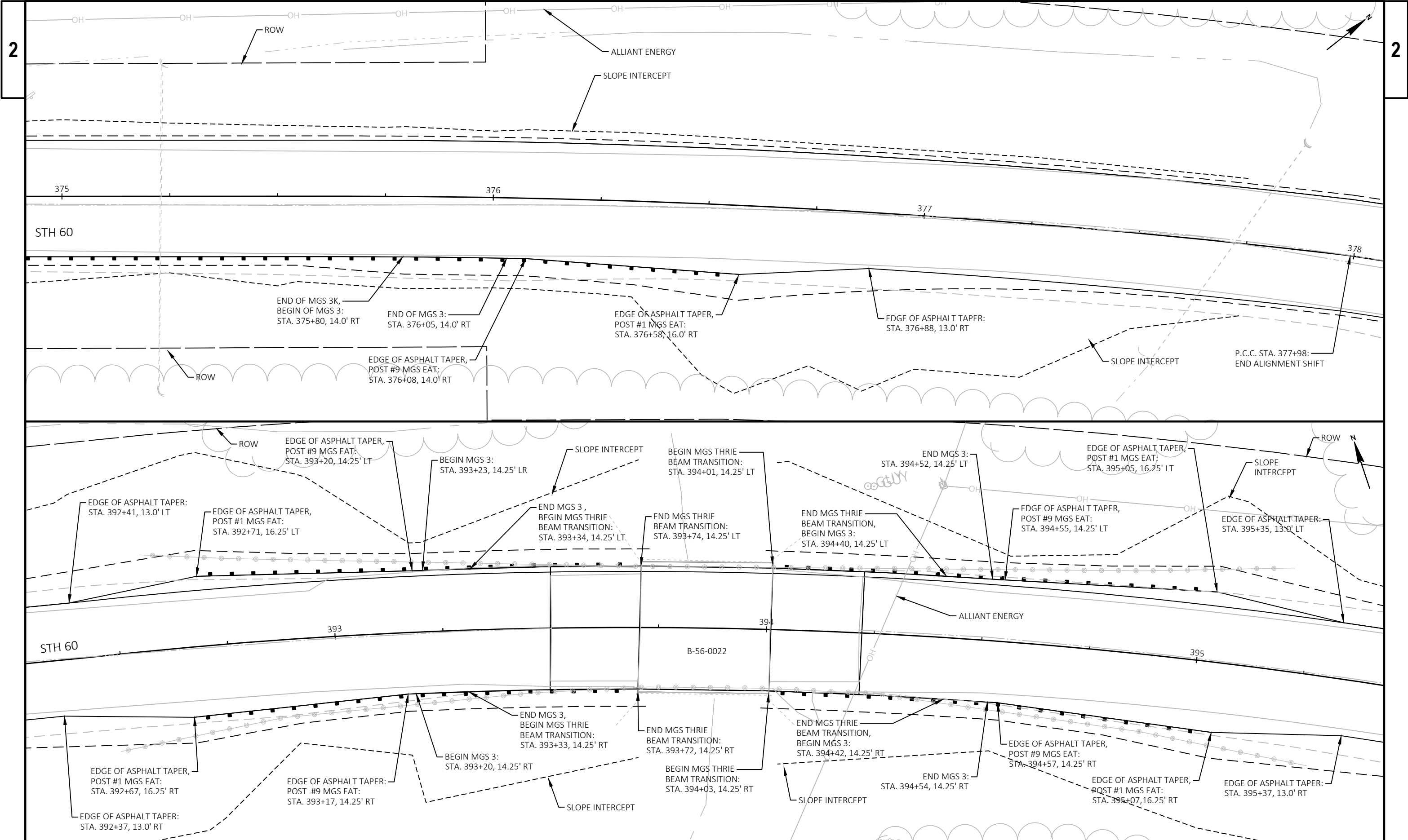
PLOT BY : SCHUMAKER, NATHANIEL

PLOT NAME :

PLOT SCALE : 1IN 20FT

WISDOT/CADDS SHEET 44

Attachment 3: Plan & Construction Details



PROJECT NO: 5680-04-60	HWY: STH 60	COUNTY: SAUK	CONSTRUCTION DETAIL	SHEET	E
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INDEX OF SHEETS

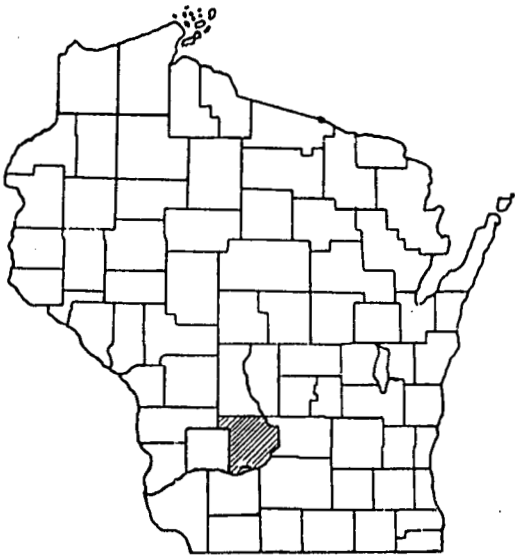
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- SHEET NO.2-2.2 TYPICAL CROSS SECTIONS
- SHEET NO.3-3.1 ESTIMATE OF QUANTITIES
- SHEET NO. 3A MISCELLANEOUS QUANTITIES
- SHEET NO.4.0-4.2 RIGHT OF WAY PLAT
- SHEET NO.5-15 PLAN AND PROFILE STA. 235+00 TO STA. 626+00
- SHEET NO.16-28 STANDARD DETAILS
- SHEET NO.29-32 DRAINAGE STRUCTURES
- SHEET NO.33-86 CROSS SECTIONS

STATE OF WISCONSIN
STATE HIGHWAY COMMISSION OF WISCONSIN

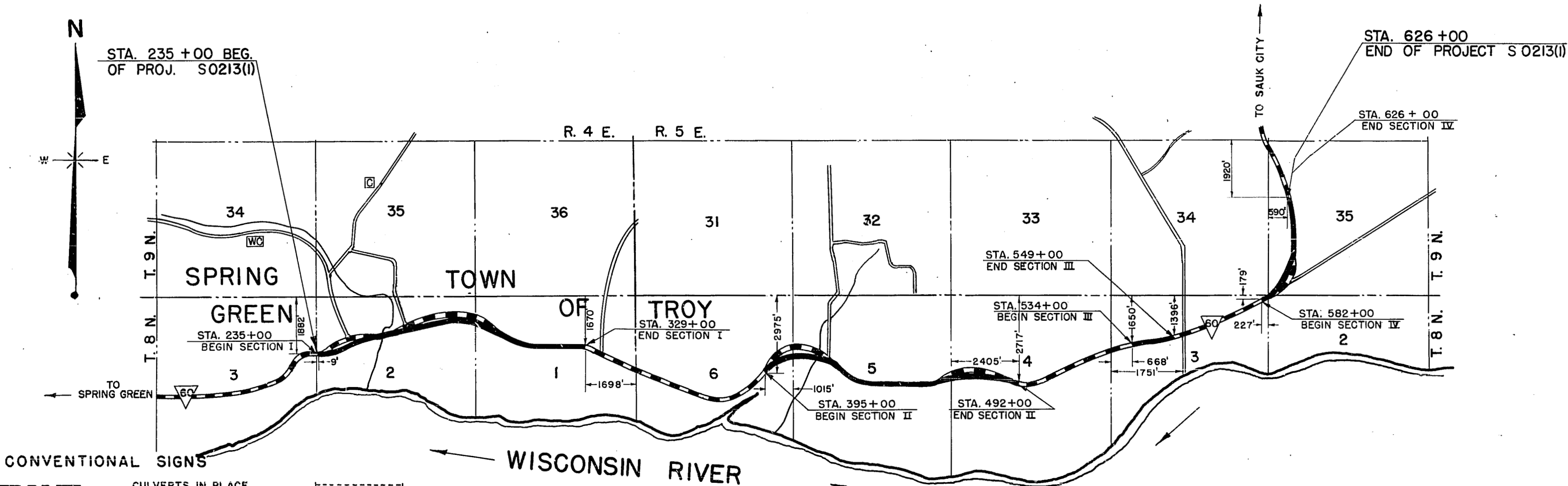
PLAN AND PROFILE OF PROPOSED
U. S. H. 14 - U.S. H. 12 ROAD
S. T. H. 60
SAUK COUNTY
PROJECT S 0213(I)

COUNTY AND HIGHWAY	ROUTE AND SECTION	CLASS AND AGREEMENT		FEDERAL DIVISION OFFICE	SHEET NUMBER	TOTAL SHEETS
		STATE	FEDERAL			
56.2	213.0		11.1	WIS. 4	1	86

R1- 4/4/57



SCALES { PLAN 1 IN. = 100 FT.
PROFILE HOR. 1IN. = 100 FT. VERT. 1 IN. = 10 FT.
CROSS SECTIONS HOR. 1 IN. = 5 FT. VERT. 1 IN. = 5 FT.



CONVENTIONAL SIGNS

- STATE LINE.....
- COUNTY LINE.....
- TOWNSHIP OR RANGE LINE.....
- SECTION LINE.....
- NEW RIGHT OF WAY LINE.....
- PRESSENT RIGHT OF WAY LINE.....
- WIRE FENCE { WOVEN.....
- BARBED.....
- LOT LINE.....
- CORPORATE OR CITY LIMITS.....
- PROPERTY LINE.....
- TRAVELED WAY OR P.E.....
- RAILROADS.....
- BASE OR SURVEY LINE.....

- CULVERTS IN PLACE.....
- CULVERTS REQUIRED.....
- DROP INLET.....
- POWER POLE.....
- TELEPHONE OR TELEGRAPH POLE.....
- RIGHT OF WAY MARKERS.....
- REFERENCE STAKE FOR HUBS ONLY.....
- MARSH.....
- HEDGE.....
- TREES.....
- GROUND ELEVATION..... DATUM LINE 73.9
- GRADE ELEVATION..... DATUM LINE 75.16

LAYOUT

SCALE 0 1/4 1/2 1.0 MILE

TOTAL NET LENGTH OF CENTERLINE	= 4.735	MI.
SECTION I	= 1.780	MI.
SECTION II	= 1.837	MI.
SECTION III	= 0.284	MI.
SECTION IV	= 0.834	MI.

FENNER-BREY ENGINEERING CORP.
MANITOWOC & BEAVER DAM

BY *J. E. Fenner*
SIGNATURE

STATE HIGHWAY
COMMISSION OF WISCONSIN
MADISON, WISCONSIN

CORRECT:
DATE *3/12/56* *J. E. Fenner*
DISTRICT ENGINEER

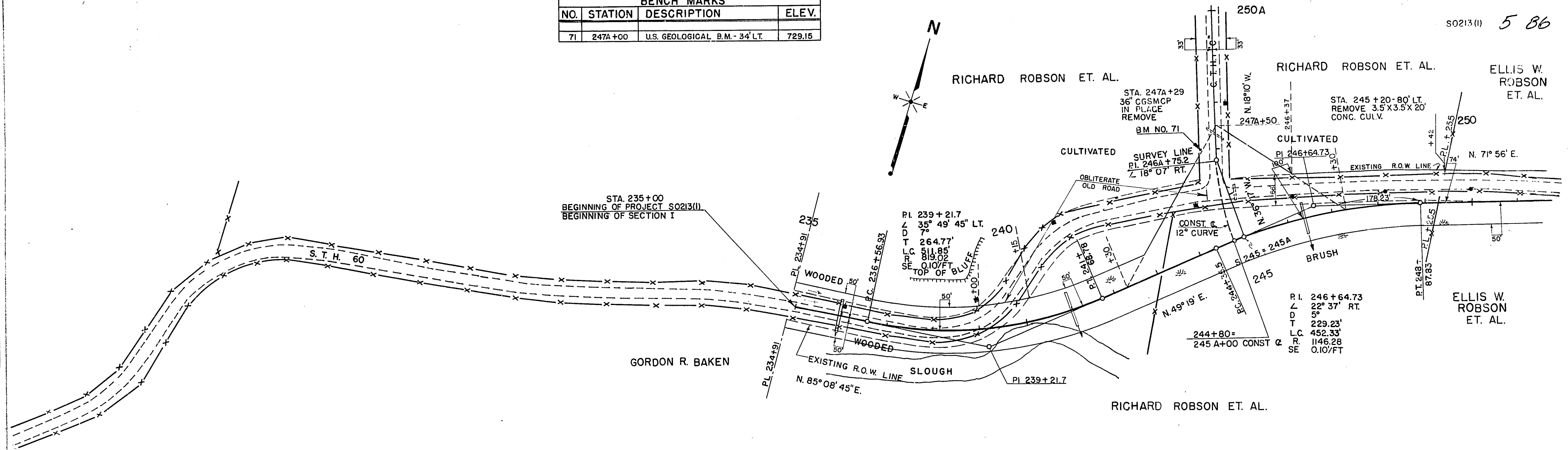
RECOMMENDED FOR APPROVAL:
DATE *3/15/56* *J. E. Fenner*
ENGINEER OF DESIGN

APPROVED:
DATE *3/15/56* *E. C. Rostig*
STATE HIGHWAY ENGINEER

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS

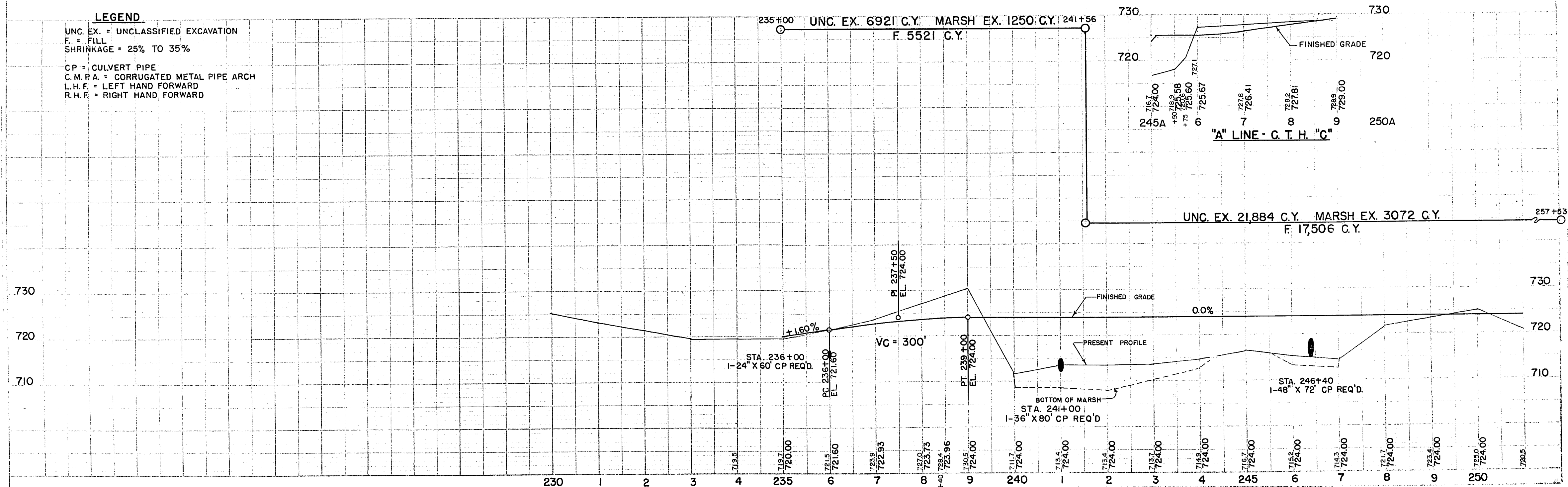
APPROVED: _____ DATE _____
DISTRICT ENGINEER

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
71	247A+00	U.S. GEOLOGICAL B.M. - 34' LT.	729.15

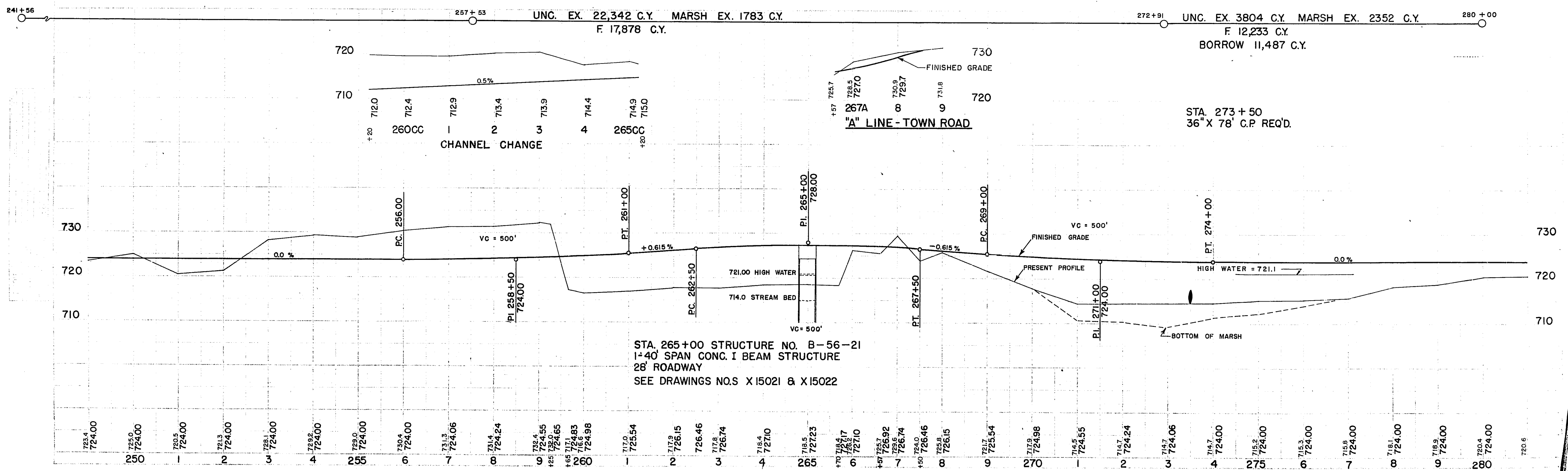
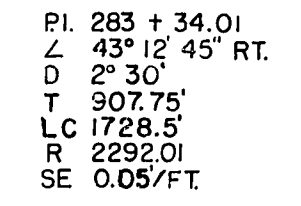


LEGEND

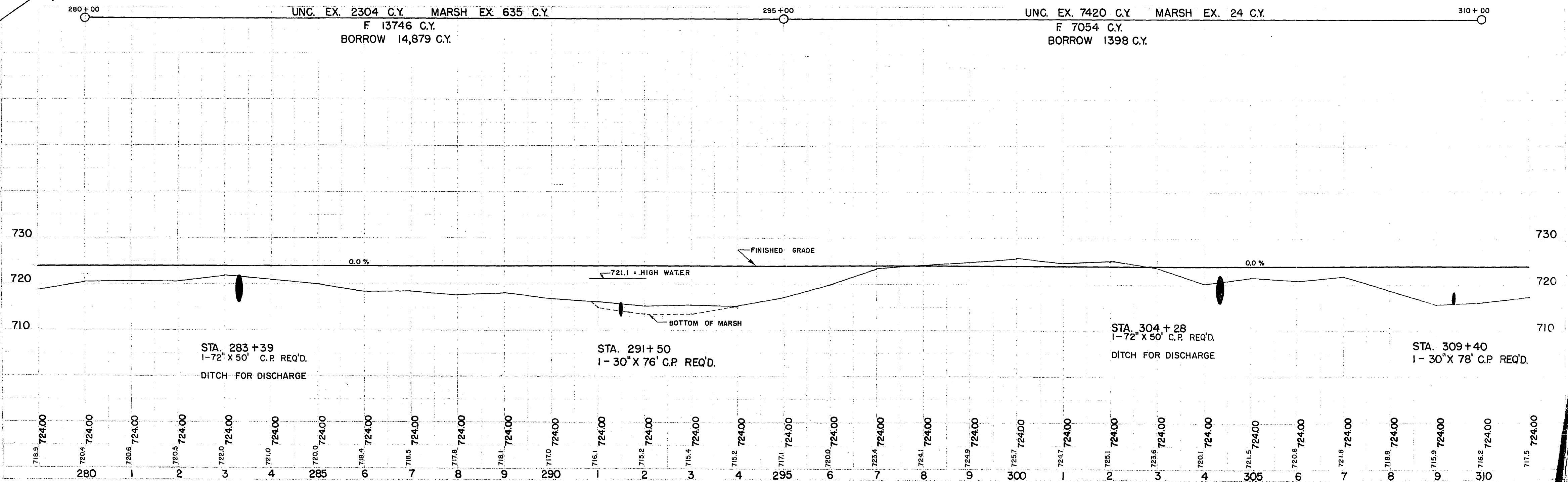
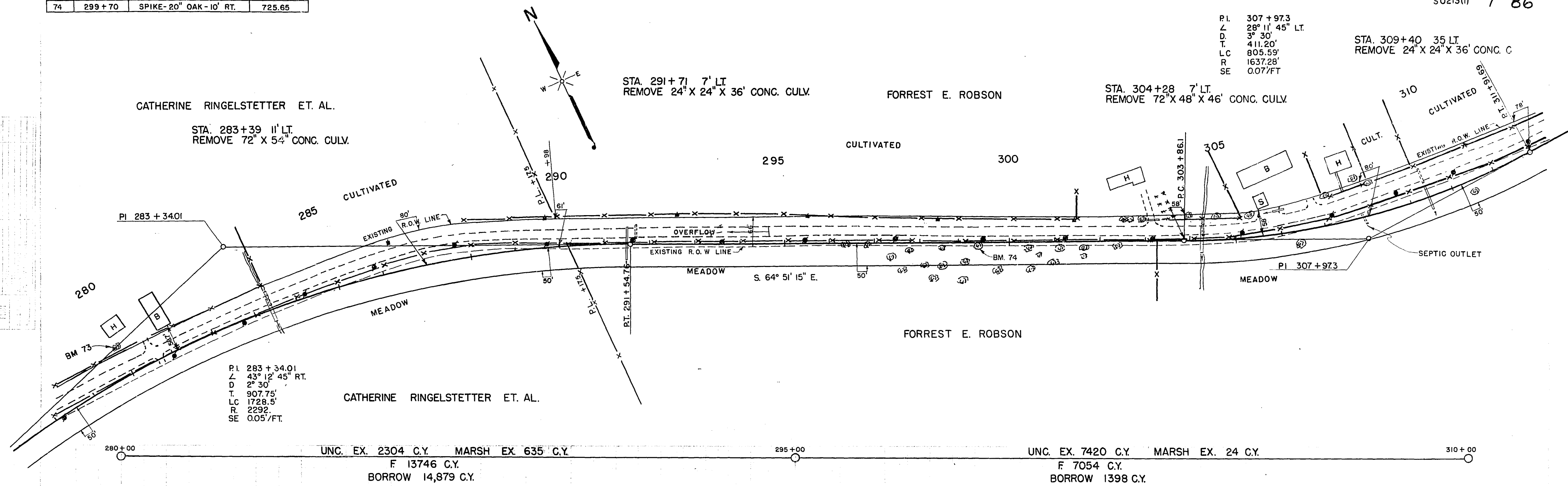
UNC. EX. = UNCLASSIFIED EXCAVATION
 F. = FILL
 SHRINKAGE = 25% TO 35%
 CP = CULVERT PIPE
 C.M.P.A. = CORRUGATED METAL PIPE ARCH
 L.H.F. = LEFT HAND FORWARD
 R.H.F. = RIGHT HAND FORWARD



ELLIS W. ROBSON
ET. AL.



BENCH MARK			
NO.	STATION	DESCRIPTION	ELEV.
74	299 + 70	SPIKE - 20" OAK - 10' RT.	725.65



BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
75	312 + 83	SPIKE - 18" B. ELDER - 74' LT.	720.84
76	333 + 40*	SPIKE - PP - 45' LT.	726.67

* TRAVERSE STATION

P.I. 307 + 97.3
 L 28° 11' 45" LT.
 D. 3° 30'
 T. 411.20'
 L.C. 805.59'
 R. 1637.28'
 SE 0.07'/FT.

FORREST E. ROBSON

EARL WILLIAMS

EARL WILLIAMS

STA. 318 + 66 12' RT.
 REMOVE 24" X 34' C.G.S.M.C.P.

STA. 323 + 32 8' RT.
 REMOVE 8' X 45' CONC. CULV.

W E

320

325

STA. 329 + 00
 END OF SECTION I

TOWN ROAD

BM 76

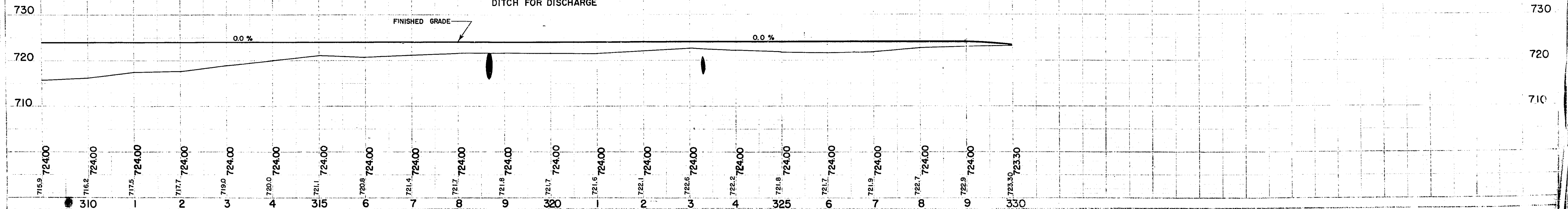
TEMPORARY APPROACH

EARL WILLIAMS

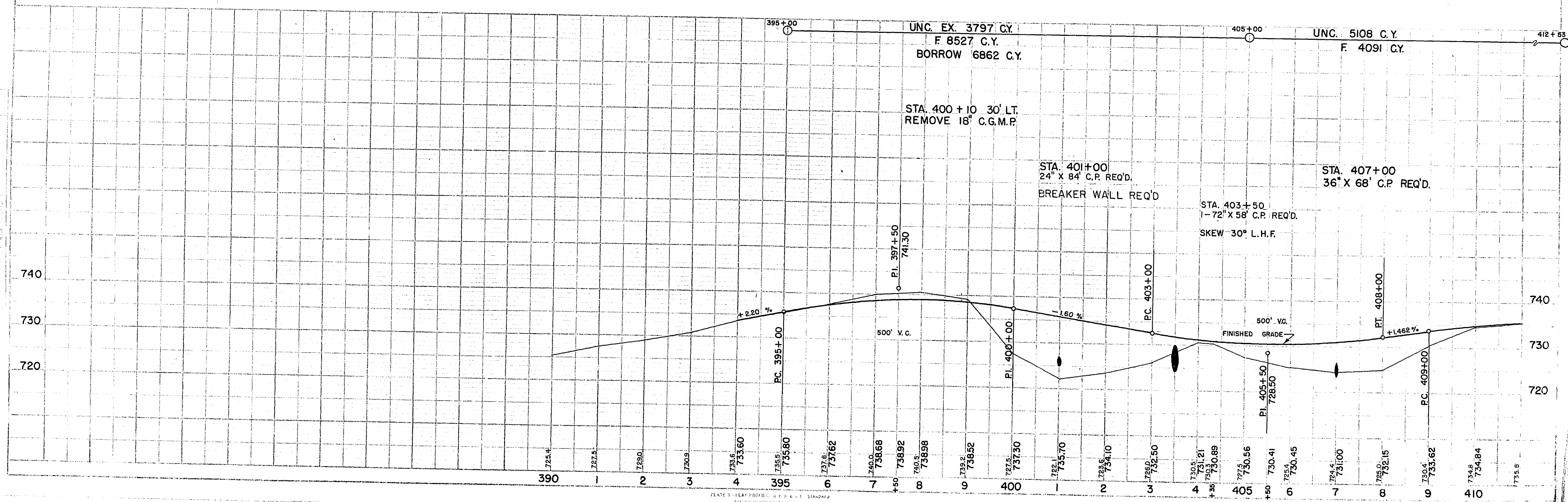
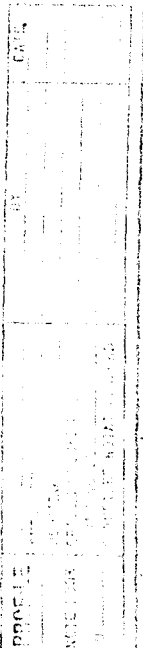
UNC. EX. 2978 C.Y.
 F 10248 C.Y.
 BORROW 9832 C.Y.

STA. 318 + 66
 1- 72" X 48' CULV. PIPE REQ'D.
 DITCH FOR DISCHARGE

STA. 323 + 32
 1- 65" X 40" X 60' C.M.P.A. REQ'D.



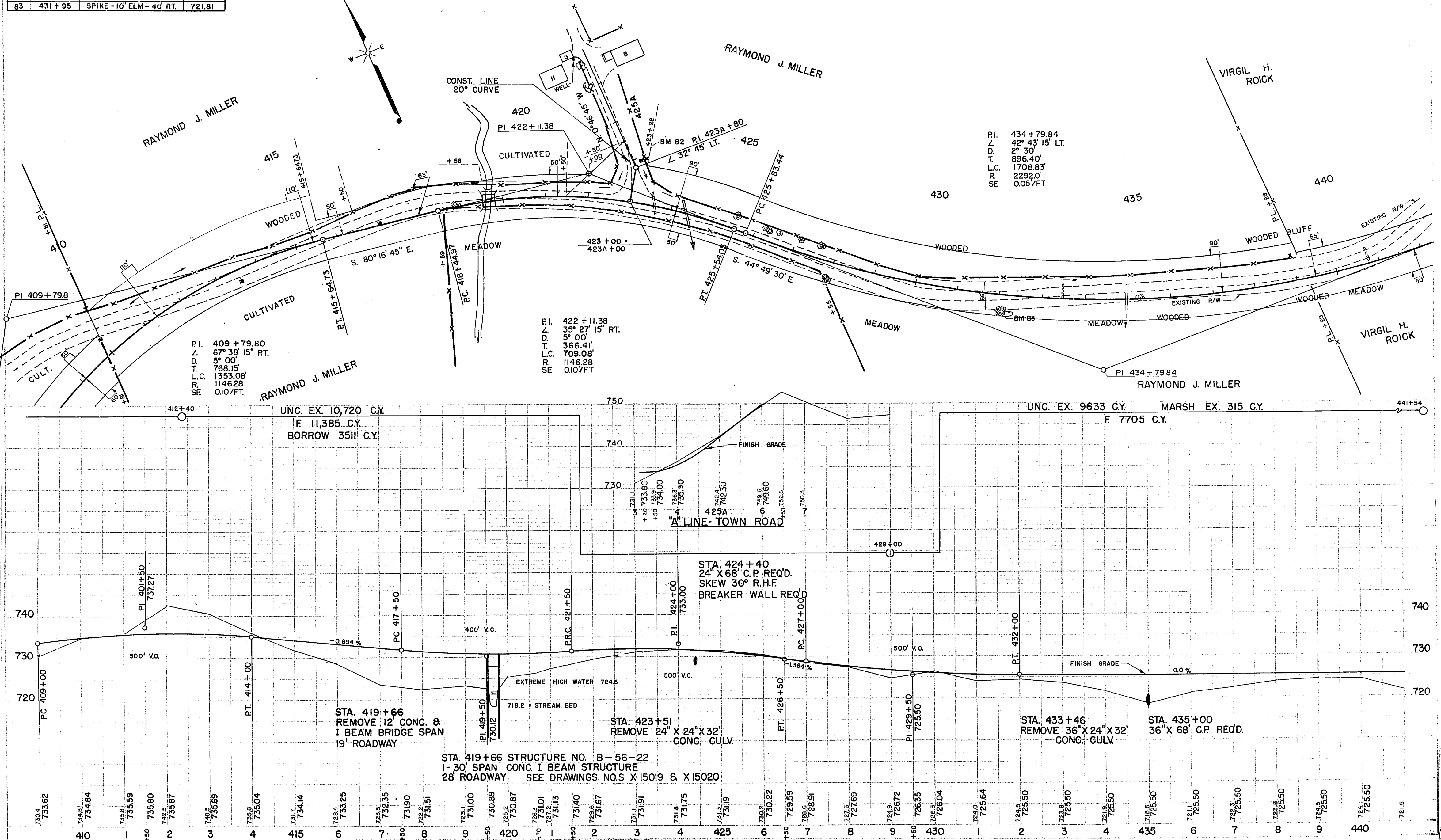
S 0213(1) 9 86



BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
82	423A+90	U.S. BM - 25' RT.	736.44
83	431+95	SPIKE - 10' ELM - 40' RT.	721.81

NO.	STATION	DESCRIPTION	ELEV.
82	423A+90	U.S. BM - 25' RT.	736.44
83	431+95	SPIKE - 10' ELM - 40' RT.	721.81

NO.	STATION	DESCRIPTION	ELEV.
82	423A+90	U.S. BM - 25' RT.	736.44
83	431+95	SPIKE - 10' ELM - 40' RT.	721.81



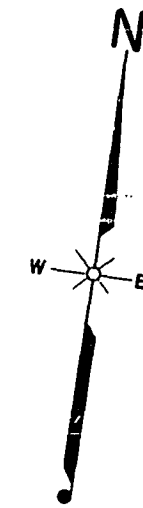
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
84	445+40	SPIKE - 16" ELM - 150' LT.	723.39
85	456+42	SPIKE - 12" ELM - 4' LT.	722.04
86	467+88	SPIKE - 36" ELM - ON G.	723.29

VIRGIL H. ROICK

P.I. 434+79.84
 L 42° 43' 15" LT.
 D. 2° 30'
 T. 896.40'
 L.C. 1708.83'
 R. 2292.01'
 SE 0.05/FT

RAYMOND J. MILLER

OBLITERATE OLD ROAD



P.I. 458+59.59
 L 41° 31' 45" LT.
 D. 5° 00'
 T. 434.63'
 L.C. 830.58'
 R. 1146.28'
 SE 0.10/FT

VIRGIL H. ROICK

FRANK SCHROFER
PI 471+61.87

OBLITERATE OLD ROAD 470

P.I. 471+61.87
 L 56° 44' 30" RT.
 D. 5° 00'
 T. 619.02'
 L.C. 1134.83'
 R. 1146.28'
 SE 0.10/FT

ALMON GASSER

VIRGIL H. ROICK

VIRGIL H. ROICK

441+54

UNC. EX. 3085 C.Y. MARSH EX. 7933 C.Y.
 F. 26,070 C.Y.
 BORROW 29,503 C.Y.

459+00

UNC. EX. 1015 C.Y. MARSH EX. 9933 C.Y.
 F. 21,847 C.Y.
 BORROW 26,295 C.Y.

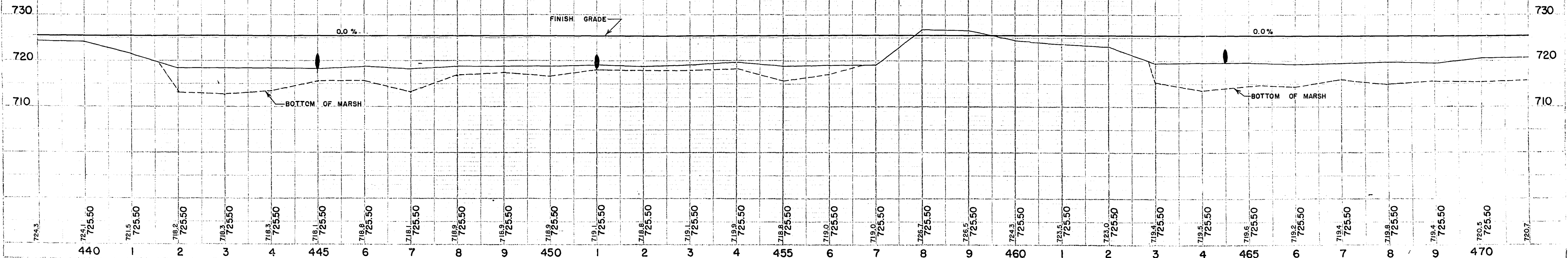
475+00

STA. 440+77 10' LT.
 REMOVE 36" X 24" X 30' CONC. CULV.

STA. 445+00
 36" X 60' C.P. REQ'D.

STA. 451+00
 36" X 58' C.P. REQ'D.

STA. 465+00
 36" X 56' C.P. REQ'D.



BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
87	478 + 87	SPIKE - 24" MPL. - 70' RT.	722.76
88	492 + 70	SPIKE - 24" ELM - 10' LT.	721.44

P.I. 471+61.87
 L 56° 44' 30" RT.
 D. 5° 00'
 T. 619.02'
 L.C. 1134.83'
 R. 1146.28
 SE 0.10/FT

FRANK SCHROFER

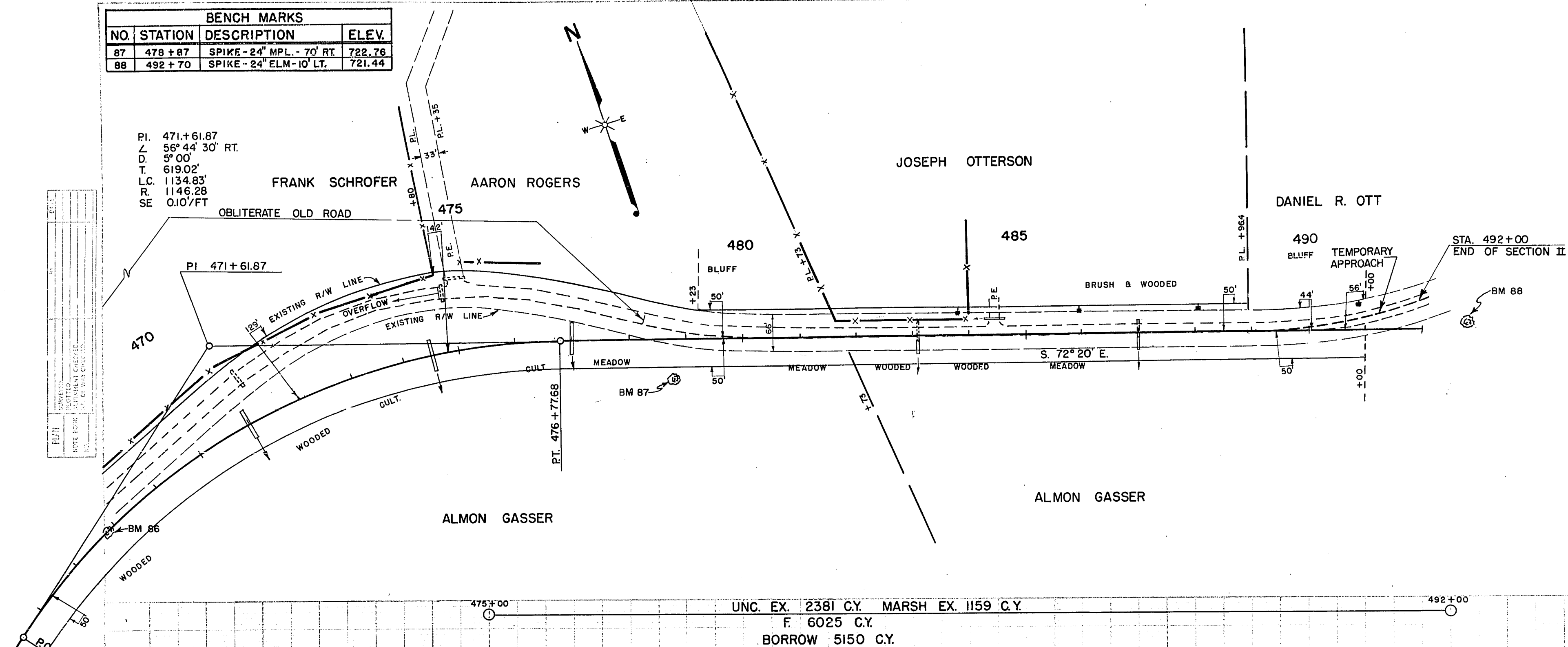
AARON ROGERS

JOSEPH OTTERSON

DANIEL R. OTT

OBLITERATE OLD ROAD

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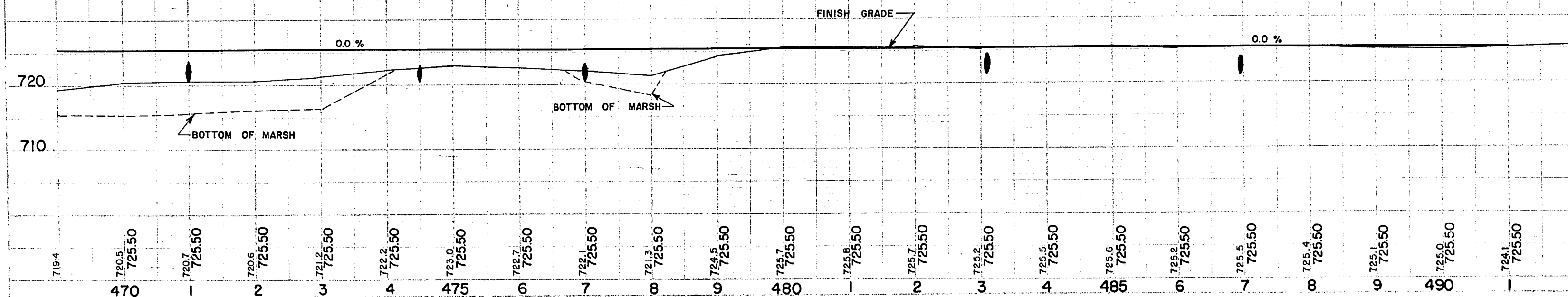
UNC. EX. 2381 C.Y. MARSH EX. 1159 C.Y.
 F. 6025 C.Y.
 BORROW 5150 C.Y.

STA. 486+97
 48" X 24" X 32' CONC. CULV. IN PLACE
 EXTEND RT. WITH 43" X 27" X 20' C.M.P.A.

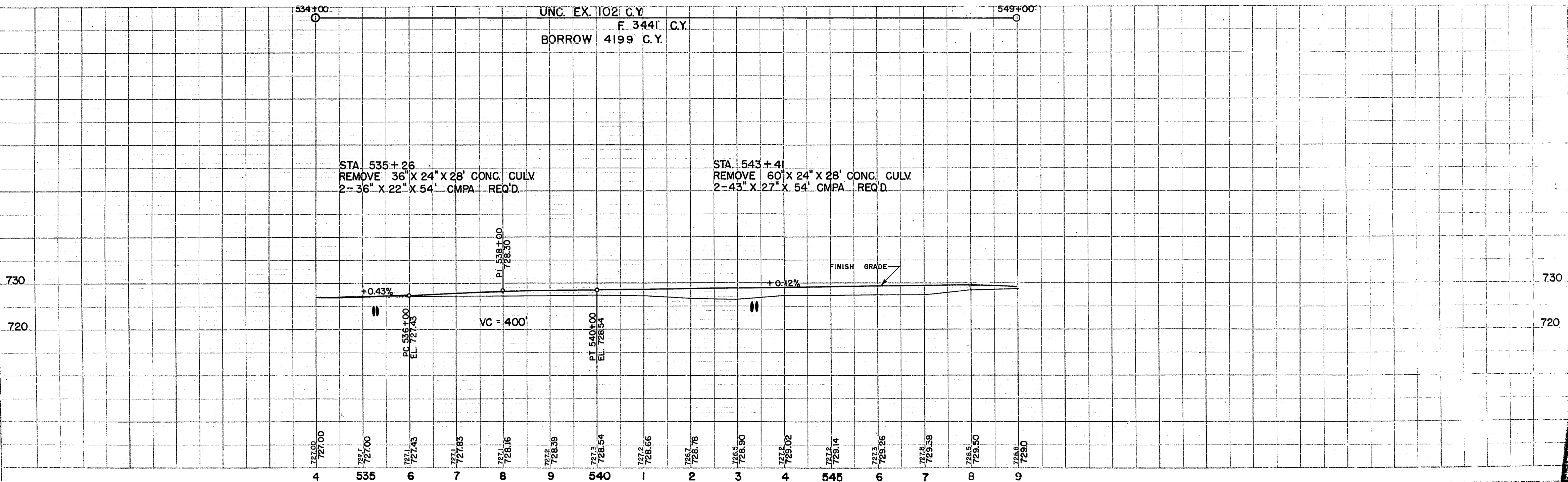
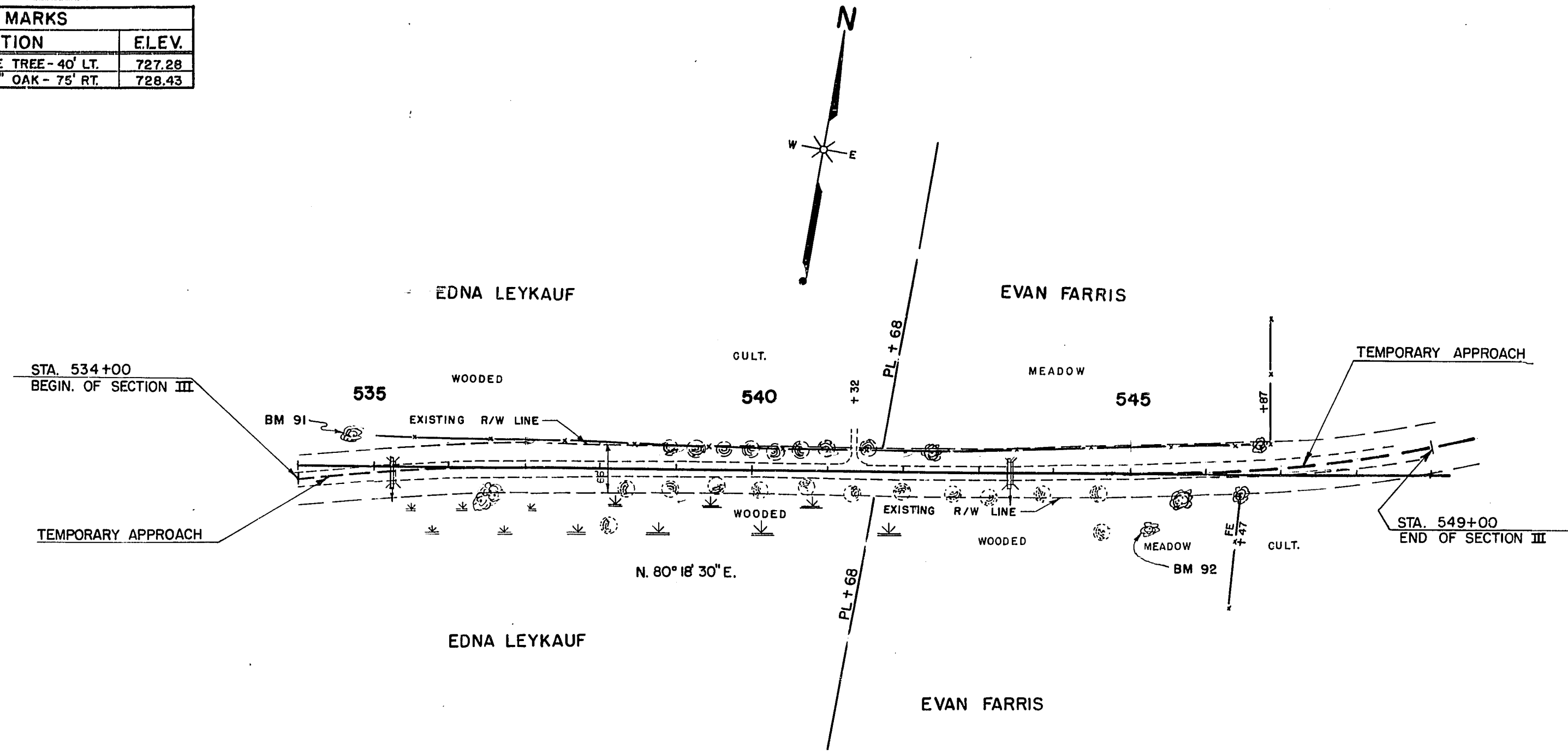
STA. 471+00
 1- 58" X 36" X 56' CMPA REQ'D.

STA. 474+50
 1- 43" X 27" X 56' CMPA REQ'D.
 DITCH FOR DISCHARGE

STA. 483+10
 48" X 24" X 32' CONC. CULV. IN PLACE
 EXTEND 20' RT. WITH 43" X 27" X 20' C.M.P.A.



BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
91	534 + 70	SPIKE - TIE TREE - 40' LT.	727.28
92	545 + 25	SPIKE - 36" OAK - 75' RT.	728.43

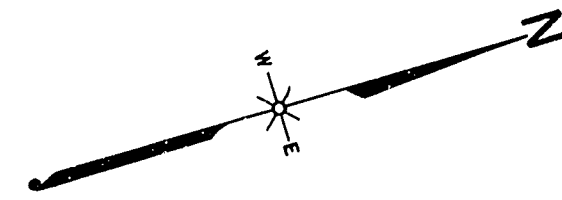


PLAN
NO. 1
DATE
BY
SURVEYED
NOTES
CHECKED
BY
DATE

PROFILE
NO. 1
DATE
BY
SURVEYED
NOTES
CHECKED
BY
DATE

P.I. 595 + 00
 Z 64° 21' 30" LT.
 D. 5° 00'
 T. 721.27'
 L.C. 1287.17'
 S.E. 0.10/FT.

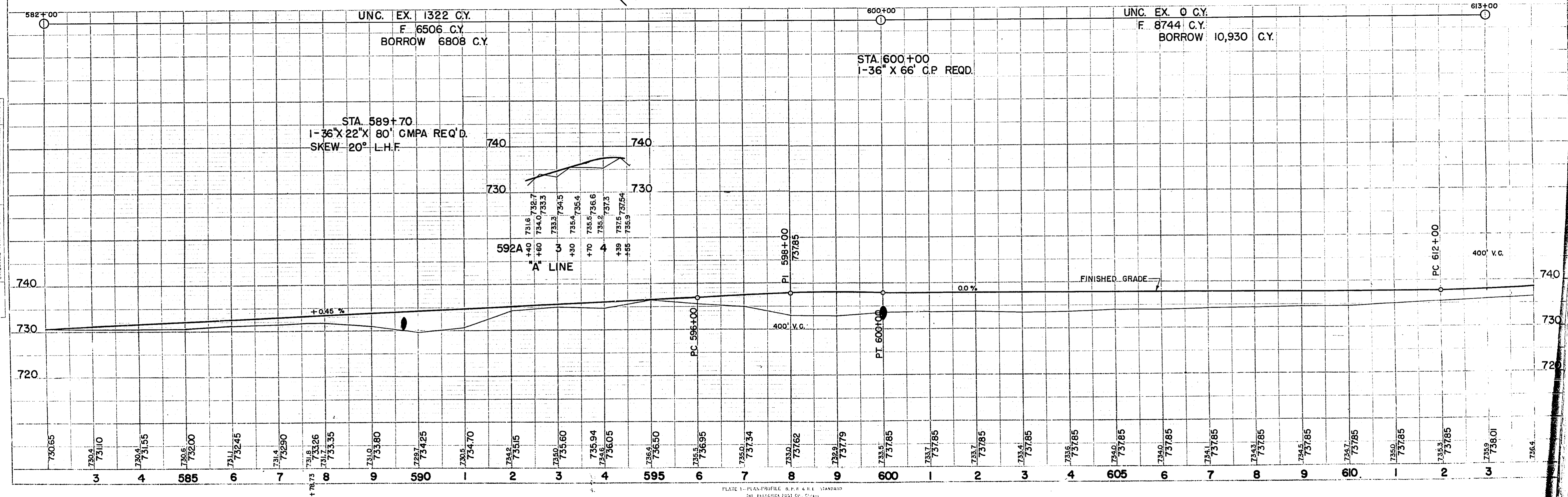
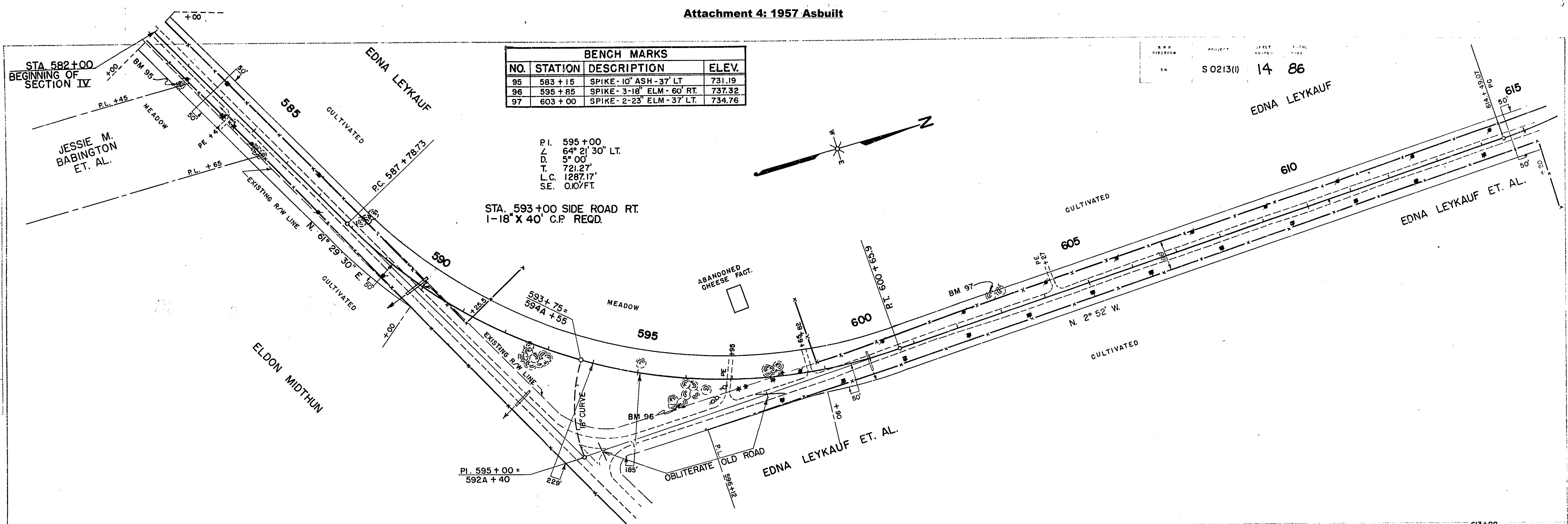
STA. 593+00 SIDE ROAD RT.
1-18" X 40' C.P. REQD.



MEADOW

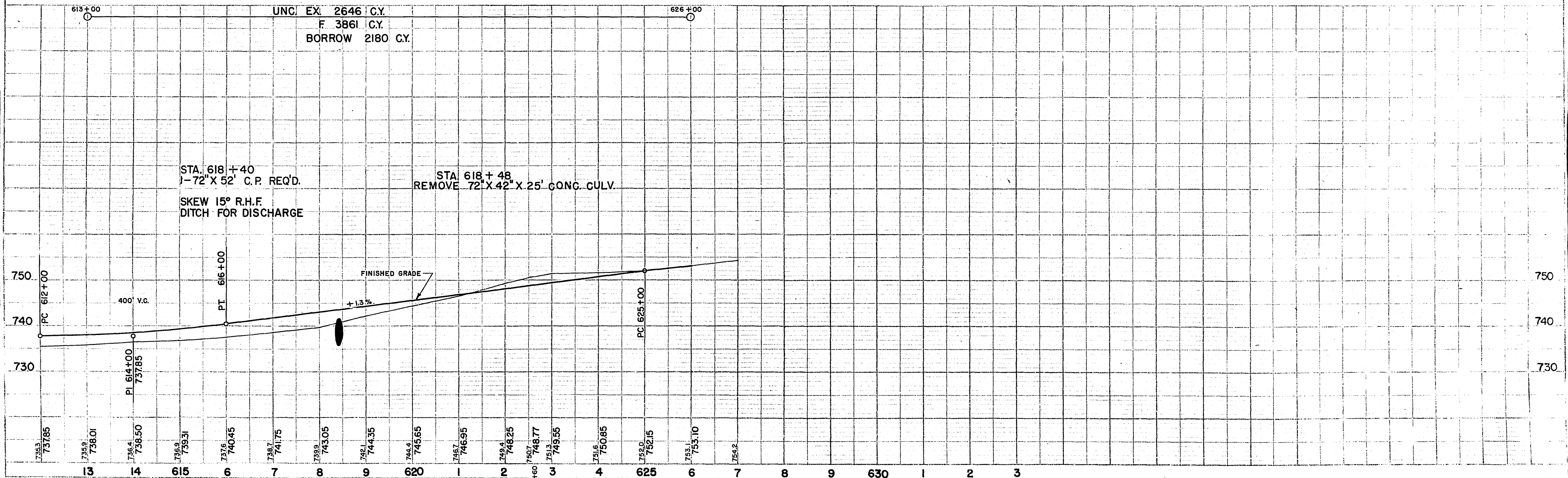
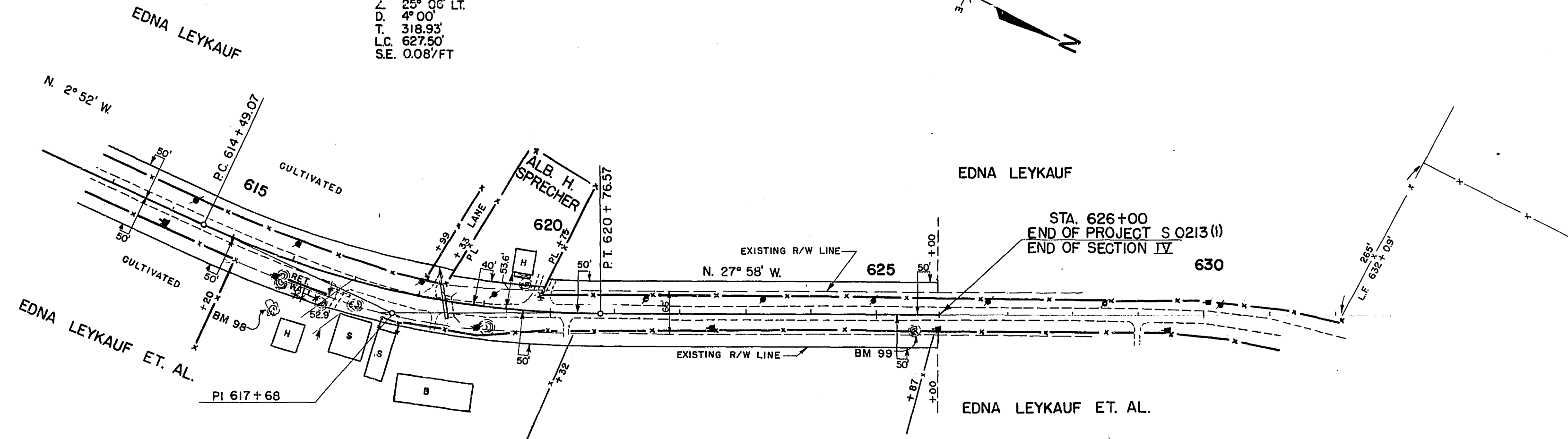
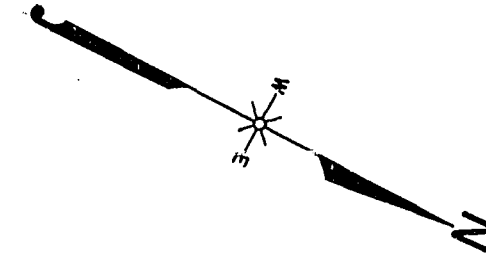
OBLITERATE OLD ROAD

EDNA LEYKAUF ET. AL.



BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
98	616+00	SPIKE-30" ELM-80' RT.	738.52
99	625+56	SPIKE-3-7" GRAB APPLE-22' RT.	754.07

PI. 617+68
 Z. 25° 06' LT.
 D. 4° 00'
 T. 318.93'
 L.C. 627.50'
 S.E. 0.08/FT





Attachment 5: Wetland Impact Tracking Form

Wisconsin Department of Transportation

Revised 12/2018

Division of Transportation System Development

WETLAND IMPACT TRACKING FORM

****This form must be filled out for all projects.****

Return This Completed Form to:

Steve Vetsch
Environmental Coordinator
WisDOT-SW Region
3550 Mormon Coulee Rd
La Crosse, WI, 54601
Phone: (608) 785-9049
Stephan.Vetsch@dot.wi.gov

**Please Complete All Information
Highlighted In Yellow**

**WisDOT Regional
Environmental Coordinator
(REC) Will Complete Sections
Highlighted In Green**

Project Design I.D. #: 5680-04-30
Project Construction I.D. #: 5680-04-60
Hwy/Project Title : Spring Green - Sauk City
USH 14 to Fox Road
County : Sauk
Construction Year : 2020
Let Date: 4/14/2019
Date this form is completed: 7/26/2019
Date this form is approved: 8/1/2019

This Form Prepared by:

Nathan Schumaker 608-789-5538 nathaniel.schumaker@dot.wi.gov
NAME PHONE EMAIL

This Form Approved by:

Steve Vetsch (608) 785-9049 stephan.vetsch@dot.wi.gov
NAME PHONE EMAIL

Is a discharge of dredged or fill material into wetlands anticipated?

NO ☐ ➔ Form complete; no further information is required (RETURN FORM TO REC).

YES ☒ ➔ 1. Complete remainder of form:

After final wetland impacts are determined, complete yellow portions on both pages of this form and submit to REC for finalization and approval. Also provide a copy of wetland impact displays.

2. Include this final APPROVED form with DNR 401 request and USACE 404 permit application.

3. Provide a PDF copy of the USACE 404 permit and DNR 401/final concurrence letter to REC.

Wetland Delineation/

Determination completed by:

Andrew Barta 608-275-3308 andrew.barta@wisconsin.gov
NAME PHONE EMAIL

DNR Liaison
QUALIFICATIONS

Describe methods used to avoid and minimize impacts to wetlands:

Select borrow will be used for steeper slopes and MGS 3K will be implemented to avoid unnecessary fill into wetlands.

Was professional discretion
used to determine debit
ratio?

No

Yes



Describe discretionary
rationale below:

**WETLAND IMPACT / REPLACEMENT
SUMMARY**

Type Impacted	Area Impacted	Type Mitigated	Area Mitigated
AB	-	AB	-
BOG	-	BOG	-
DM	-	DM	-
M	-	M	0.10
RPE	0.03	RPE	-
RPF	0.04	RPF	-
SM	-	SM	-
SS	-	SS	-
WS	-	WS	-
AB(D)	-	TOTAL	0.10
DM(D)	-		
M(D)	-		
RPE(D)	-		
RPF(D)	-		
SM(D)	-		
SS(D)	-		
WS(D)	-		
TOTAL	0.07		

Division of Transportation System Development

WETLAND IMPACT TRACKING FORM - PAGE 2

DETAILED TABLE OF WETLAND IMPACTS

Notes for Page 2 completion:

1. A wetland area (ID) may be made up of multiple wetland types. Separate the impact area by type and report in separate rows.
2. To add additional rows, right click on row number within the table and select "insert". Repeat as needed.
3. Use Department of Transportation Wetland Classification System. See abbreviations tab.
4. Individual wetland impacts should be reported to the **nearest 0.001-acre**.
5. The Environmental Coordinator will enter the appropriate debit ratio, mitigation type, area, and bank information.
6. Impacts and mitigation are automatically summed by type and rounded to the nearest 0.01-acre. See page 1.

[illegible]

Is there potential for onsite mitigation? If unknown, check with the REC.

YES

11/11/2019

Where is it located? (T/R, station, map)

NO

X

List bank site to be used. **(Determined by REC)**

Neptune-Richland

Please attach another sheet if the space provided is not adequate for all impacts or to add any additional comments.

National Flood Hazard Layer FIRMette

Attachment 6: FEMA Flood Maps



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance Water Surface Elevation
		17.5 Cross Sections with 1% Annual Chance Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
OTHER FEATURES		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature
MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped



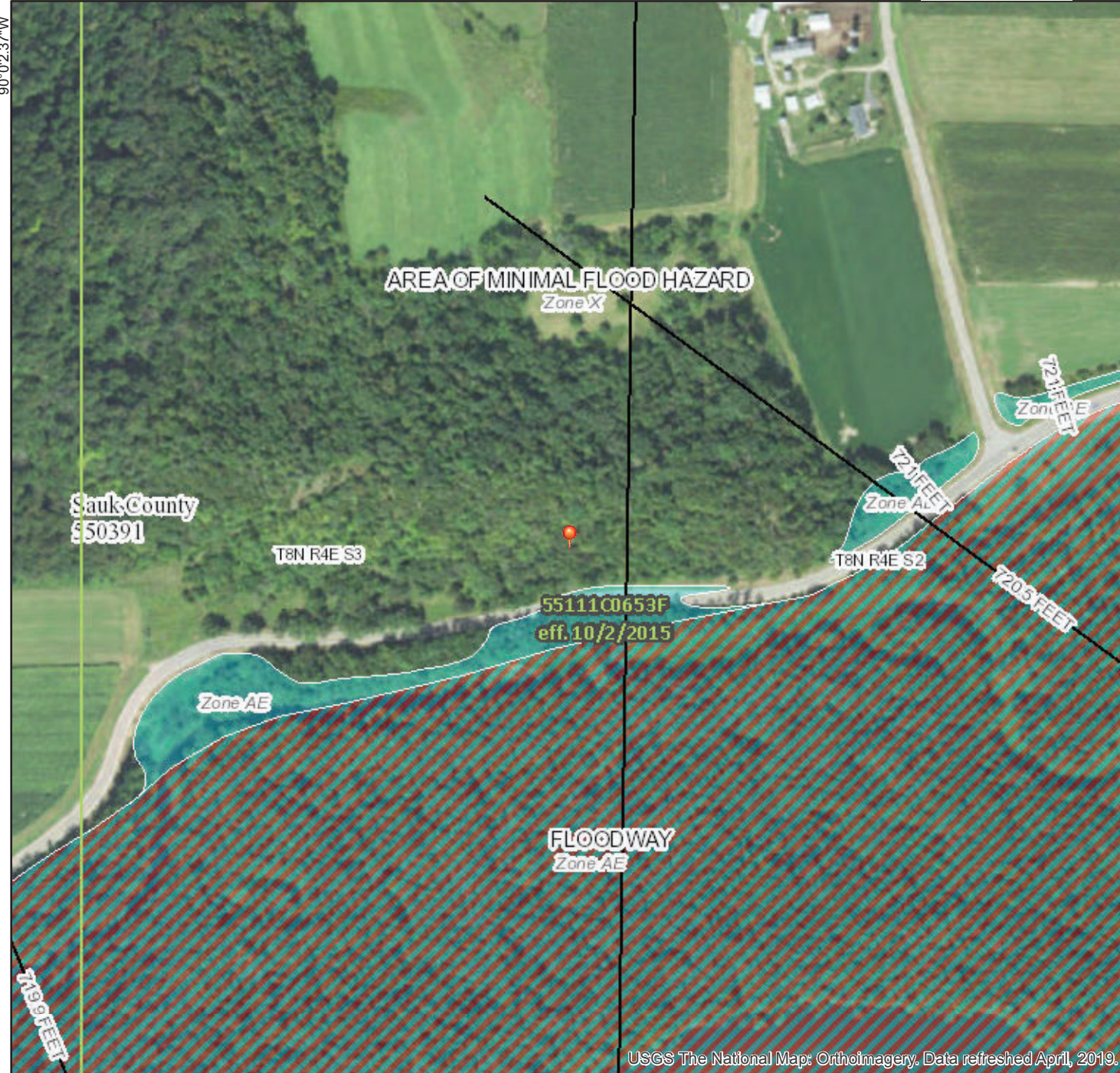
The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 7/26/2019 at 10:56:03 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

43°12'19.06"N



USGS The National Map: Orthoimagery. Data refreshed April, 2019.

0 250 500 1,000 1,500 2,000 Feet 1:6,000

43°11'52.83"N

89°59'24.91"W

National Flood Hazard Layer FIRMette

Attachment 6: FEMA Flood Maps



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
OTHER FEATURES		Levee, Dike, or Floodwall
		Cross Sections with 1% Annual Chance Water Surface Elevation
MAP PANELS		20.2
		17.5
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature
		Digital Data Available
		No Digital Data Available
		Unmapped



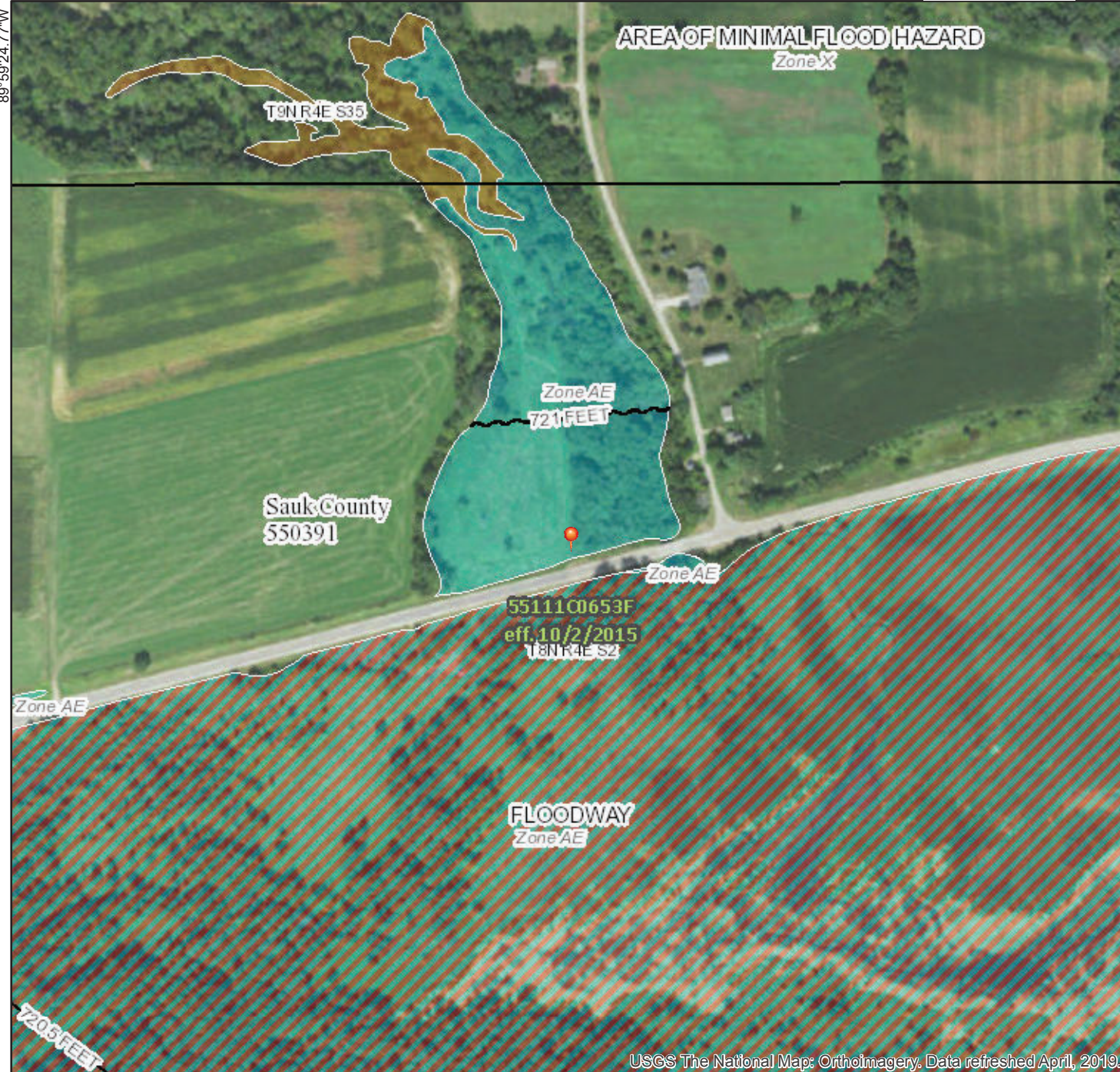
The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 7/26/2019 at 10:57:45 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

43°12'27.08"N



USGS The National Map: Orthoimagery. Data refreshed April, 2019.

43°12'0.85"N

0 250 500 1,000 1,500 2,000 Feet 1:6,000

89°58'47.32"W

National Flood Hazard Layer FIRMette

Attachment 6: FEMA Flood Maps



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
		Area of Undetermined Flood Hazard Zone D
GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance Water Surface Elevation
		17.5
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
MAP PANELS		Profile Baseline
		Hydrographic Feature
		Digital Data Available
		No Digital Data Available
		Unmapped



The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 7/26/2019 at 11:07:52 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

43°12'7.53"N



USGS The National Map: Orthoimagery. Data refreshed April, 2019.

0 250 500 1,000 1,500 2,000 Feet 1:6,000

43°11'41.31"N

W.60°10'05.69"



National Flood Hazard Layer FIRMette

Attachment 6: FEMA Flood Maps



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance Water Surface Elevation
		17.5
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
MAP PANELS		Profile Baseline
		Hydrographic Feature
		Digital Data Available
		No Digital Data Available
		Unmapped

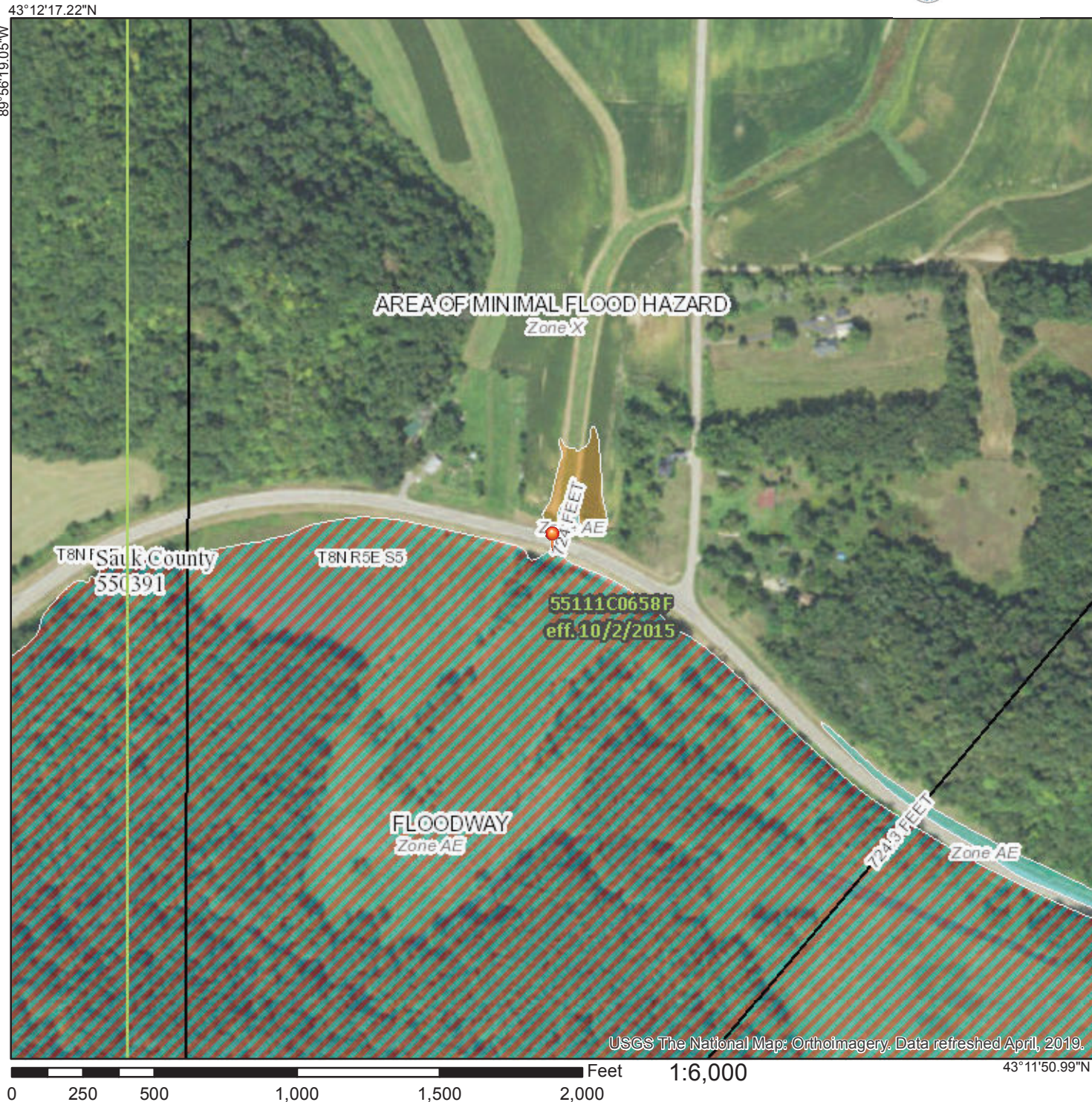


The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 7/26/2019 at 10:59:55 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.



Attachment 7: WDNR Initial Comments Letter

State of Wisconsin
DEPARTMENT OF NATURAL RESOURCES
101 S. Webster Street
Box 7921
Madison, WI 53703-7921

Scott Walker, Governor
Daniel L. Meyer, Secretary
Telephone 608-266-2621
Toll Free 1-888-936-7463
TTY Access via relay - 711



5-31-18

Nathaniel Schumaker
WisDOT DTSD
3550 Mormon Coulee Rd
La Crosse WI 54601

— **Subject: DNR Initial Project Review**

Project I.D. 5680-04-30
STH 60
Spring Green – Sauk City
Sauk County
S02, T08N, R04E

Dear Mr. Schumaker:

The Wisconsin Department of Natural Resources (DNR) has received the information you provided for the above-referenced project. According to your proposal, the purpose of this project is to mill and relay pavement. Proposed improvements include mill and relay HMA, shoulder work, and beam guard treatments.

Preliminary information has been reviewed by DNR staff for the project under the DNR/DOT (Wisconsin Department of Transportation) Cooperative Agreement. Initial comments on the project as proposed are included below, and we assume that additional information will be provided that addresses all resource concerns identified. To ensure compliance with resource protections, we are recommending that Special Provisions be developed for specific resource protections described below. DNR expects that the full range of DOT roadway standards will be applied throughout the design and construction process.

A. Project-Specific Resource Concerns

Section 4(f) Requirement:

Public lands are present in the vicinity of this project. If there is potential for impacts to these lands, please begin coordination with us as soon as possible. *First and foremost, every effort should be taken to avoid impacts to these lands.*

There is a U.S. Dept. of Transportation "Section 4(f)" process for federally funded transportation projects that impact various types of public parks, wildlife refuges, and recreation areas. This requirement is coordinated by state and federal transportation departments. Please ensure the 4f process is followed according to the DOT facilities development manual.

This project proposes no right-of-way easements or aquitions. No impacts are anticipated.

Wetlands:

There is potential for wetland impacts to occur as a result of this project. Wetland impacts must be avoided and/or minimized to the greatest extent practicable. Unavoidable wetland losses must be compensated for in accordance with the DNR/DOT Cooperative Agreement and the DOT Wetland Mitigation Banking Technical Guideline. DNR requests information regarding the amount and type of unavoidable wetland impacts.

A preliminary Wetland Impact Tracking form has been completed, and it has been attached. A total of 0.07 acres of RPE and RPF wetlands will be mitigated to the Neptune site in Richland County.

Fisheries/Stream Work:

It is our understanding that no in-water or culvert work is planned for this project. We do not anticipate impacts to fisheries or aquatic systems.

If erosion control matting is to be used along stream banks or adjacent to wetland areas, DNR recommends biodegradable non-netted matting (e.g. Class I Type A Urban, Class I Type B Urban, or Class II Type C). Long-term netted mats may cause animal entrapment. Avoid the use of fine mesh matting that is tied or bonded at the mesh intersection such that the openings in the mesh are fixed in size.

WisDOT is using Urban Class I Type B erosion mat in all disturbed areas.

Aquatic Connectivity and Culvert Work:

It is our understanding that any culvert work will be handled by maintenance. ***All culvert replacements by the maintenance program or the County Highway Department should be coordinated with DNR.*** Any culvert work prior to the project should take the following considerations into account.

Culverts should be set and sized in such a manner to avoid or minimize impacts to stream morphology, aquatic organism passage, and water quality. The invert elevation of the new culvert(s) should be set an adequate distance below the natural streambed elevation, to allow for a natural and continuous streambed condition to occur. The invert elevations of the existing and proposed structure(s), the water surface elevations, and the natural streambed elevations upstream and downstream should be specified in the plans.

Culverts are being replaced or extended in-kind by county forces to minimize impacts to stream morphology and aquatic organisms. WisDOT continues to coordinate with county forces ensuring they have been addressing WDNR's concerns.

Endangered Resources:

Based upon a review of the Natural Heritage Inventory (NHI) and other DNR records dated 5-23-18, the following Endangered Resources have been known to occur in the project area or its vicinity and could be impacted by this project.

Rusty Patched Bumble Bee (*Bombus affinis*), listed as Federally Endangered and Special Concern in Wisconsin, is extremely rare in Wisconsin and is considered both state- and globally-imperiled. This bee relies on diverse and abundant flowering plant species in proximity to suitable overwintering sites for hibernating queens. Portions of this project also fall within the High Probability Zone (HPZ)

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The following measures will be needed: Since the project is mostly contained to pavement replacement and regularly maintained shoulder areas for beam guard work, we don't anticipate adverse impacts to this species. However, we do recommend seeding any disturbed areas with native seed mix #70A.

The WisDOT will use seed mix #70A in all disturbed areas.

Loggerhead Shrike (*Lanius ludovicianus*), a bird listed as Endangered in Wisconsin, prefers open country with scattered trees and shrubs (usually hawthorne and red cedar), and edge habitat such as open areas in forests. The recommended avoidance period is from **April 15 - August 15**.

The following measures will be needed: In order to protect active nests for this species and other migratory birds, all tree and brush clearing should occur between August 15th and April 15th outside of the active nesting season.

If clearing & grubbing becomes necessary, WisDOT will have county forces perform the work outside the active nesting season stated above.

Narrow-leaved Dayflower (*Commelina erecta* var. *deamiana*), a Wisconsin Special Concern plant, is found in sand prairies and on sandstone outcrops, mostly along the lower Wisconsin River. Blooming occurs late June through early September; fruiting occurs late August through late September. The optimal identification period for this species is late June through early September.

The following measures will be needed: DNR staff will perform visual surveys of ROW areas with appropriate habitat during the summer of 2018. Any populations identified that could be impacted by this project will need to be relocated. DNR has several nearby properties that could accommodate this.

Purple Milkweed (*Asclepias purpurascens*), a Wisconsin Endangered plant, is found in open oak forests, forest margins, and roadsides. Blooming occurs early June through late July; fruiting occurs early July through late August. The optimal identification period for this species is late June through late July.

The following measures will be needed: DNR staff will perform visual surveys of ROW areas with appropriate habitat during the summer of 2018. Any populations identified that could be impacted by this project will need to be relocated. DNR has several nearby properties that could accommodate this.

The WDNR completed a site review in July of 2019. They determined there were no signs of Narrow-Leaved Dayflower or Purple Milkweed.

The Transportation Liaison will initiate coordination with DNR Conservation Biologist, Stacy Rowe.

- ❖ ***NHI Disclaimer:** This review letter may contain NHI data, including specific locations of endangered resources, which are considered sensitive and are not subject to Wisconsin's Open Records Law. As a result, information contained in this review letter may be shared only with individuals or agencies that require this information in order to carry out specific roles in the permitting, planning and implementation of the proposed project. Specific locations of endangered resources may not be released or reproduced in any publicly disseminated documents.*

Migratory Birds:

To avoid impacts to nesting birds, removal of trees and shrubs which are likely to support active nests, or ground disturbance and vehicle traffic in grasslands with potential ground-nesting migratory birds should be completed between August 15th and April 15th.

If clearing & grubbing becomes necessary, WisDOT will have county forces perform the work outside the active nesting season stated above.

Invasive Species and Viral Hemorrhagic Septicemia (VHS):

If any work is to occur in waterways or wetlands all project equipment shall be decontaminated for removal of invasive species prior to and after each use on the project site by utilizing other best management practices to avoid the spread of invasive species as outlined in NR 40, Wis. Adm. Code. For more information, refer to <http://dnr.wi.gov/topic/Invasives/bmp.html>.

The STP for aquatic exotic species control has been added to the project's special provisions.

- **Emerald Ash Borer:** This project has the potential for spreading the Emerald Ash Borer (EAB) beetle. It is illegal to move or transport ash material, the emerald ash borer, and hardwood debris (i.e. firewood) from EAB quarantined areas to a non-quarantined area without a compliance agreement issued by WI Department of Agriculture, Trade and Consumer Protection. Regulated items include cut hardwood (non-coniferous) firewood, ash logs, ash mulch or bark fragments larger than one inch in diameter, or ash nursery stock (DATCP statute 21).
 - For more information regarding the EAB and quarantine areas please click on the following link: <http://datcpservices.wisconsin.gov/eab/article.jsp?topicid=20>
 - Recommendations to reduce the spread of EAB in potentially infested Ash wood: <http://datcpservices.wisconsin.gov/eab/articleassets/Recommendations%20to%20reduce%20the%20spread%20of%20EAB.pdf>
- **Oak Wilt:** This project involves work that may involve cutting or wounding of oak trees. To prevent the spread of oak wilt disease, please avoid cutting or pruning of oaks from April 1 through September 30. For more information and guidance see the DNR webpage at: <http://dnr.wi.gov/topic/foresthhealth/oakwilt.html>.

If clearing & grubbing becomes necessary, WisDOT will have county forces perform the work outside the active nesting season stated above.

Floodplains:

A determination must be made as to whether the project lies within a mapped/zoned floodplain. Any proposed temporary or permanent changes to the road or waterway geometry in mapped floodplain areas requires that DOT coordinate with the Sauk Zoning Administrator to ensure compliance with the local zoning ordinance and NR116. Examples of floodplain development activity includes, but not limited to, the following: changes to waterway crossings; culvert extensions; changes to road surface elevations and/or side-slopes; temporary causeways; temporary structures; general fill.

- A preliminary review of the Surface Water Data Viewer (SWDV) indicates that floodplain conditions exist within the project limits.

The project area is adjacent to a mapped floodplain. There are no proposed changes to the waterway geometry, and there are minor changes to the horizontal geometry. From sta. 361 to sta. 377, WisDOT

is proposing to shift the alignment north by up to two feet to minimize fill impacts to the wetland along the south side of STH 60. The north side of this section is outside the mapped floodplain.

Storm Water Management & Erosion Control:

- For projects disturbing an acre or more of land, erosion control and storm water measures must adhere to the Wisconsin Pollutant Discharge Elimination System Transportation Construction General Permit (TCGP) for Storm Water Discharges. Coverage under TCGP is required prior to construction. DOT should apply for permit coverage just before the project goes to final PS&E. Permit coverage will be issued by the DNR after design is complete and documentation shows that the project will meet construction and post-construction performance standards. For more information regarding the TCGP you can go to the following link, and click on the “Transportation” tab: <https://dnr.wi.gov/topic/Sectors/Transportation.html>.
- All projects require an Erosion Control Plan (ECP) that describes best management practices that will be implemented before, during and after construction to minimize pollution from storm water discharges. Additionally, the plan should address how post-construction storm water performance standards will be met for the specific site. The project design and Erosion Control Implementation Plan (ECIP) must comply with the TCGP in order to receive “permit-coverage” from the DNR.
- Once the project contract has been awarded, the contractor will be required to outline their construction methods in the ECIP. An adequate ECIP for the project must be developed by the contractor and submitted to this office for review at least 14 days prior to the preconstruction conference. For projects regulated under the TCGP, submit the ECIP as an amendment to the ECP.

The WisDOT will apply for and adhere to the TCGP, and per standard specifications, will require the contractor to submit an ECIP 14 days prior to the preconstruction conference.

Selected Site & Commercial Non-Metallic Mines:

- The DOT Select Site process must be adhered to for clean fill or any other material that leaves the work site. The DNR liaison will review all proposed select sites and a site visit may be required. Filling of wetlands, waterways or floodplain is not allowed under the select site process, unless the site owner obtains required permits. No new impermeable surfaces can be left at a select site (including gravel roads or pads), unless the site owner obtains required permits. Contaminated materials leaving the site need to adhere to the Hazardous Material Management Plan.
- Use of Commercial Non-Metallic Mines must accompany documentation that such mines have received all applicable local, state and federal permits before being used on the project, including local non-metallic mining reclamation permits and applicable WPDES permits as issued by the DNR.

As part of the ECIP review, WisDOT will coordinate the approval of selected sites with the WDNR.

Special Features:

- **Seeding and Mulching Recommendation:**
 - DNR is requesting that seed mix No. 70A be used on this project, due to proximity to Lower Wisconsin Riverway property and high quality habitats utilized by threatened and endangered

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species, and to satisfy requirements for Rusty Patch Bumble Bee and the RPBB High Probability Zone.

- DNR is requesting that weed-free mulch be used for restoration of this project site, due to proximity to Lower Wisconsin Riverway property and high quality habitats utilized by threatened and endangered species.. Some weed-free mulch suppliers can be found at: <http://wcia.wisc.edu/mulch.pdf>.

WisDOT is proposing to use seed mix #70A and Urban Class I Type B erosion mat in all disturbed areas.

Unofficial Boat Launch:

DOT has asked that DNR comment on the small recreational boat launch site located west of Troy Village Road. This is an unofficial launch point that can be used by the public to launch small craft, primarily canoes and kayaks, however it is not advertised as such nor is it maintained by DNR. To our knowledge this is not a widely or frequently used launch point, but for those that do use it, it can be an important component of their recreational endeavor.

The DNR fully understands the safety concerns associated with this curve and if DOT data and safety standards necessitate the installation of beam guard at this location, we support that action. However, if it is possible to provide shoulders with adequate room for shoulder parking similar to the current conditions, if not in the same location, DNR feels that could provide a good compromise to allow river users an opportunity to access this stretch of public lands. We understand this might not be a viable option given other physical constraints and safety concerns, but we look forward to exploring the idea further.

Due to the limited right-of-way, steep slopes & sensitive wetland areas, the shoulder could not be widened and/or guardrail could not be relocated to maintain the pull off for the boat launch. WisDOT is not proposing to close or eliminate the access to the river, but there will no longer be an accessible pad to park a vehicle.

Other Issues:

This project may require a permit from the U.S. Army Corps of Engineers (USACE). For further permit details, you may contact Kerrie Hauser of the USACE located in the LaCrescent office, at 651-290-5903. All local, state, and federal permits and/or approvals must be obtained prior to commencing construction activities.

WisDOT will work with the region's REC to coordinate the 404 permit .

The above comments represent the DNR's initial concerns for the proposed project and do not constitute final concurrence. Final concurrence will be granted after further review of refined project plans, and additional consultation if necessary. If any of the concerns or information provided in this letter requires further clarification, please contact this office at 608-275-3308, or email at andrew.barta@wisconsin.gov.

Sincerely,

Andy Barta

Andy Barta
Environmental Analysis & Review Specialist

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cc: Steve Vetsch – WisDOT Rec
Dan Klienertz - WisDOT
Kerrie Hauser – ACOE
Matt Seguin – DNR
Stacy Rower- DNR
Mark Cupp – LWSR Board