STATE PROJECT NUMBER

1003-10-84

Ⓧ INDICATES WING NUMBER

✳ LOCATION OF ANCHOR ASSEMBLY FOR FOR STEEL PLATE BEAM GUARD

N

276'-6" BACK TO BACK OF ABUTMENTS

2'-3" | 44'-11" SPAN 1 | 57'-0" SPAN 2 | 57'-0" SPAN 3 | 57'-0" SPAN 4 | 56'-1" SPAN 5 | 2'-3"

THE EXISTING STRUCTURE B-53-229 TO REMAIN IN PLACE. THE DECK WILL BE REPLACED AND THE STRUCTURE WIDENED.

B-53-229

℄ BRG. S. ABUT. STA. 735+26.03
END OF DECK STA. 735+24.78
BEGIN APPR. SLAB STA. 735+04.78

STA. 735+96.57 IH 39/90 SB
STA. 6+97.11 WSOR

℄ PIER 1 STA. 735+70.95
℄ PIER 2 STA. 736+27.95
℄ PIER 3 STA. 736+84.95
℄ PIER 4 STA. 737+41.95

STA. 737+16.38 IH 39/90 SB
= STA. 14+13.87 CTH O

℄ BRG. N. ABUT. STA. 737+98.03
END OF SLAB STA. 737+99.28
END APPR. SLAB STA. 738+19.28

3°41' SKEW (TYP.)

℄ WSOR

28'-0" | 6+00 | 26'-0 3/8"

GAS (ALLIANT)
TELEPHONE (AT&T)
FIBER OPTIC (MCLEOD)

TEMPORARY SHORING (TYP.) 2'-0" (TYP.)

PT. OF MIN. VERT. CLR. = 23'-3"

±8'-0" SHLD. | ±12'-0" ±12'-0" ASPH. PAV'T. | ±8'-0" SHLD.

13+00 | ℄ CTH O

FIBER OPTIC (MCLEOD)
ELECTRIC (ALLIANT)

CAUTION

10'-0" WING | 10'-0" WING

LIMITS OF SLOPE PAVING (TYP.)

8'-6"

FLOOR DRAIN TYPE 'WF' (TYP.)

STAGE CONST. LINE

PT. OF MIN. VERT. CLR. = 28'-9"

84'-11½" OUT-OUT OF PARAPET | 82'-0" CLEAR ROADWAY

1'-5¾"

NAME PLATE - FOR LOCATION, SEE SHEET XX.

8'-0" SHLD. | 12'-0" RAMP | 12'-0" LANE | 12'-0" LANE | 12'-0" LANE | 12'-0" LANE | 14'-0" SHLD. | 2'-0" BARRIER

℄ IH 39/90 SB

COORDINATE DETAILS WITH CMT/BOS

7+00 | 14+00 | 86°-11'-15" | 83°05'28"

735 | 736 | 737 | 738

BEGIN APPR. SLAB STA. 734+96.60

℄ IH 39/90 NB

END OF SLAB STA. 735+16.60
℄ BRG. S. ABUT. STA. 735+17.85

℄ PIER 1 STA. 735+68.77
℄ PIER 2 STA. 736+25.77
℄ PIER 3 STA. 736+82.77
℄ PIER 4 STA. 737+39.77

82°32'55"
86°-11'-15"

STA. 735+92.39 IH 39/90 NB
= STA. 7+29.36 WSOR

STA. 737+14.08 IH 39/90 NB
= STA. 14+45.94 CTH O

END OF SLAB STA. 737+97.10
℄ BRG. N. ABUT. STA. 737+95.85
END APPR. SLAB STA. 738+17.10

B-53-230

℄ WSOR

LABEL STATIONS

FLOOR DRAIN TYPE 'WF' (TYP.)

8'-6"

8'-0"

15+00

℄ CTH O

80'-0" CLEAR ROADWAY | 82'-11½" OUT-OUT OF PARAPET | 1'-5¾"

NAME PLATE - FOR LOCATION, SEE SHEET XX.

LIMITS OF SLOPE PAVING (TYP.)

2'-0" (TYP.)

TEMPORARY SHORING (TYP.)

3°41' SKEW (TYP.)

10'-0" WING | 10'-0" WING

8'-0" SHLD.

PT. OF MIN. VERT. CLR. = 23'-0"

17'-8" | 36'-4"

CAUTION

℄ CTH O

PT. OF MIN. VERT. CLR. = 27'-10"

TELEPHONE (AT&T)
FIBER OPTIC (MCLEOD)
GAS (ALLIANT)
ELECTRIC (ALLIANT)
FIBER OPTIC (MCLEOD)

1'-5¾"

2'-3" | 50'-11" SPAN 1 | 57'-0" SPAN 2 | 57'-0" SPAN 3 | 57'-0" SPAN 4 | 56'-1" SPAN 5 | 2'-3"

282'-6" BACK TO BACK OF ABUTMENTS

THE EXISTING STRUCTURE B-53-230 TO REMAIN IN PLACE. THE DECK WILL BE REPLACED AND THE STRUCTURE WIDENED.

PLAN

(5 - SPAN 36" PRESTRESSED CONCRETE GIRDERS)

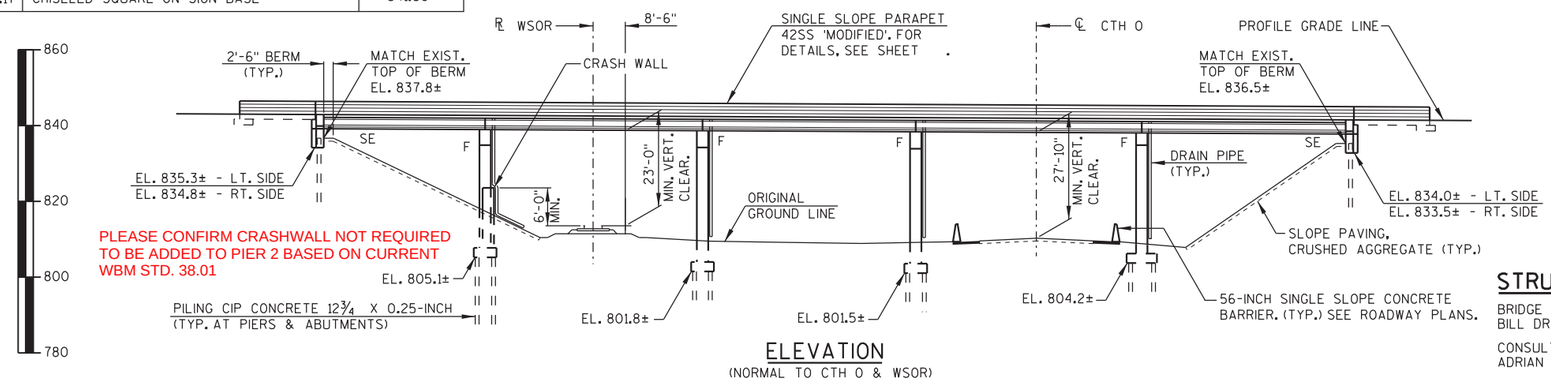
CONFIRM NOTE 9 FROM WBM STD. 38.01 HAS BEEN ADDRESSED

1'-11" ON EXISTING PLANS, COORDINATE DETAILS WITH CMT/BOS LIAISON

-M STEEL AROUND FLOOR DRAINS TO BE ADDRESSED PER PREVIOUS COORDINATION WITH CMT.

BENCH MARK TABLE

NO.	STATION	DESCRIPTION	ELEVATION
253L	745+90.17	CHISELED SQUARE ON SIGN BASE	841.80



PLEASE CONFIRM CRASHWALL NOT REQUIRED TO BE ADDED TO PIER 2 BASED ON CURRENT WBM STD. 38.01

ELEVATION

(NORMAL TO CTH O & WSOR)

DESIGN DATA

LIVE LOAD

DESIGN RATING: HL-93
INVENTORY RATING FACTOR: X.XX (Wis-SPV) WBM 6.2.2.3.4
OPERATIONAL RATING FACTOR: X.XX
WISCONSIN ~~MAX~~ STANDARD PERMIT VEHICLE ~~LOAD~~: XXX KIPS

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 PSF.

STRUCTURE IS RATED FOR A POLYMER OVERLAY OF 5 PSF. THE POLYMER OVERLAY WILL BE APPLIED TO THE ENTIRE DECK UNDER A FUTURE CONTRACT.

ULTIMATE DESIGN STRESSES (HPC?)

CONCRETE MASONRY - SUPERSTRUCTURE — f'c = 4,000 PSI
- ALL OTHER — f'c = 3,500 PSI
BAR STEEL REINFORCEMENT, GRADE 60 — fy = 60,000 PSI
36" PRE-STRESSED GIRDER f'c
CONCRETE MASONRY — f'c = 6,000 PSI
STRANDS 0.5-INCH DIA.
ULTIMATE TENSILE STRENGTH — f_{pu} = 270,000 PSI

FOUNDATION DATA

130 PER GEOTECH REPORT

ABUTMENTS AND PIERS TO BE SUPPORTED ON PILING CIP 12¾ X 0.25-INCH AND DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 160 TONS** PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 60 FEET LONG AT ABUTMENTS, 60 FEET LONG AT PIER 4 AND 80 FEET LONG AT PIERS 1, 2 & 3. PRE-BORING ESTIMATED TO BE 30 FEET AT SOUTH ABUT. AND 25 FEET AT NORTH ABUT.

**THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

PLEASE CONFIRM PILE LENGTHS ARE CONSISTENT WITH GEOTECH REPORT

CONFIRM PRE-BORING IS REQUIRED, PER WBM 11.3.1.6 - REQUIRED FOR NEW EMBANKMENTS OVER 10'

TRAFFIC DATA

CONFIRM ADT'S, DIFFERENT THAN VALUES IN SSR

IH 39/90
A.D.T. (2020) = 65,100
A.D.T. (2040) = 94,300
R.D.S. = 70 MPH

CTH O
A.D.T. (2015) = 3,900
A.D.T. (2040) = 6,300
R.D.S. = 50 MPH

LIST OF DRAWINGS

PRELIMINARY PLAN APPROVED:
BH
1/12/15

STRUCTURES DESIGN CONTACTS

BRIDGE OFFICE:
BILL DREHER (608) 266-8489

CONSULTANT:
ADRIAN KIDARSA (608) 828-8207

NO.	DATE	REVISION	BY

AECOM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED _____ _____
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-53-230

NB IH 39/90 OVER CTH O & WSOR

COUNTY ROCK | TOWN/~~CITY/VILLAGE~~ LA PRAIRIE

DESIGN SPEC. REHABILITATION N/A

DESIGNED BY	DESIGN CK'D.	DRAWN BY	PLANS CK'D.
AK	TR	TAW	TR

GENERAL PLAN

SHEET 1 OF 7

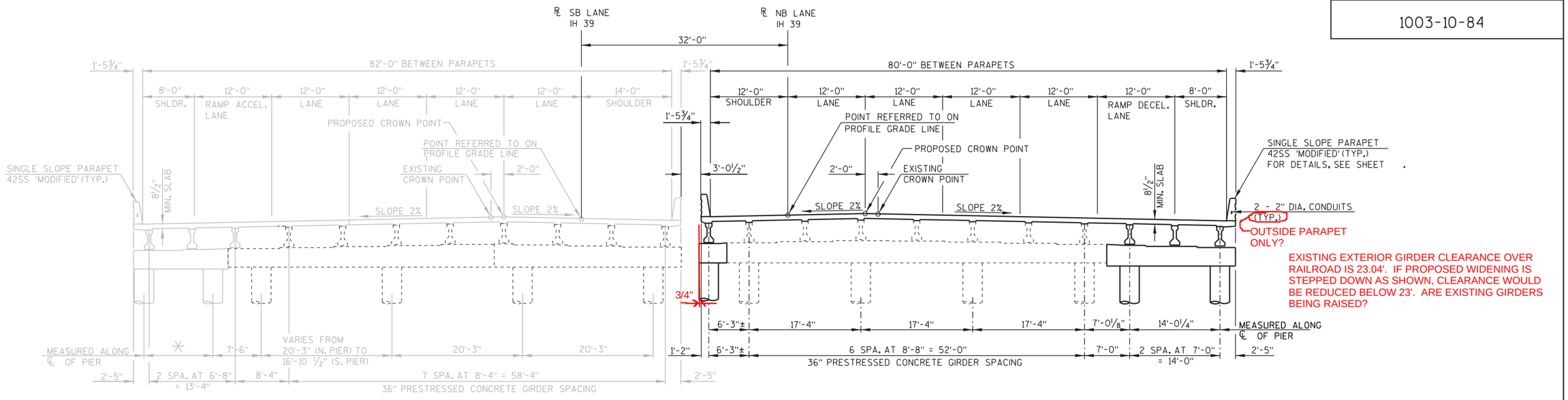
8

8

PRINTER DRIVER: S:\..acm-CADstds..Libraries\WISDOT\MicroStation\Resources\MS_Printng\Printer_Drivers\HE_PDF.11 x 17.plt
PEN TABLE: S:\..acm-CADstds..Libraries\WISDOT\MicroStation\Resources\MS_Printng\pen tables\HE_WisDOT.tbl
FILE NAME: L:\work\projects\60242867\400.Technical\408.Structural\408.1.Structure.Design\408.1.220 B-53-230_CTH O NB\408.1.220.12 Dgns\02 section_prof_b-53-230.dgn
PLOT DATE: 11/14/2014 PLOT TIME: 7:58:13 AM BATCH PRINT SHEET 2 OF 7

STATE PROJECT NUMBER

1003-10-84



B-53-229

B-53-230

EXISTING EXTERIOR GIRDER CLEARANCE OVER RAILROAD IS 23.04'. IF PROPOSED WIDENING IS STEPPED DOWN AS SHOWN, CLEARANCE WOULD BE REDUCED BELOW 23'. ARE EXISTING GIRDERS BEING RAISED?

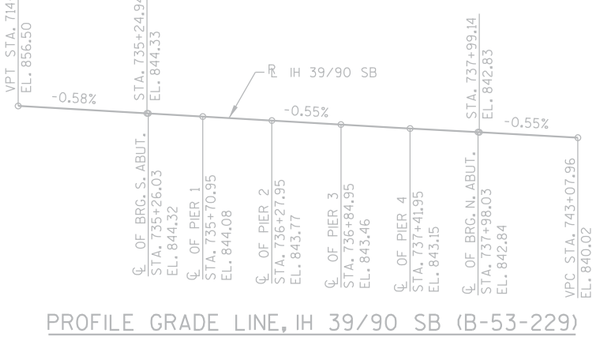
✳ VARIES FROM 9'-9"± (N. PIER) TO 13'-2"± (S. PIER)

CROSS SECTION THRU ROADWAY

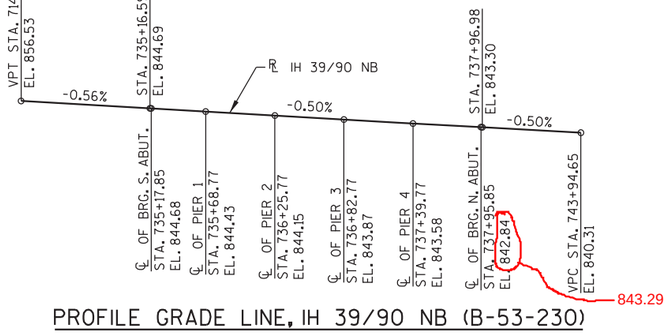
(LOOKING NORTH)

CONFIRM EXISTING PIERS/ABUTMENTS HAVE SUFFICIENT CAPACITY FOR CURRENT DESIGN LOADS AND ARE SUFFICIENT TO PROVIDE FOR A 75-YEAR DESIGN LIFE AS NOTED IN THE I39 CORRIDOR MANUAL

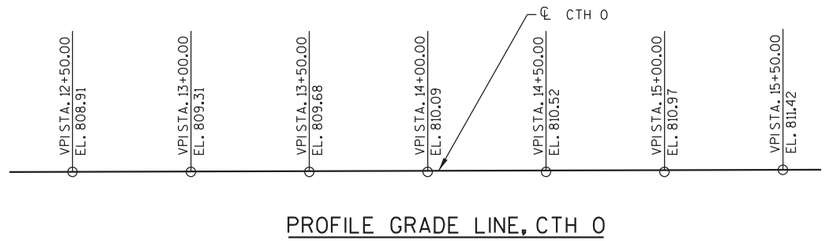
NOT CONSISTENT WITH PLAN AND PROFILE, PLEASE CONFIRM



PROFILE GRADE LINE, IH 39/90 SB (B-53-229)

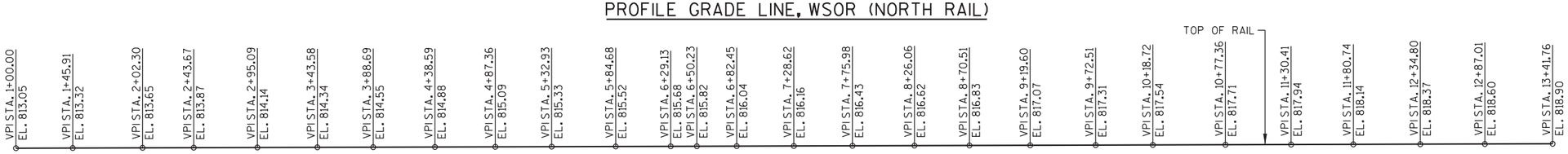


PROFILE GRADE LINE, IH 39/90 NB (B-53-230)



PROFILE GRADE LINE, CTH O

PROFILE GRADE LINE, WSOR (NORTH RAIL)



PROFILE GRADE LINE, WSOR (SOUTH RAIL)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-53-230

DRAWN BY TAW | PLANS CK'D. TR

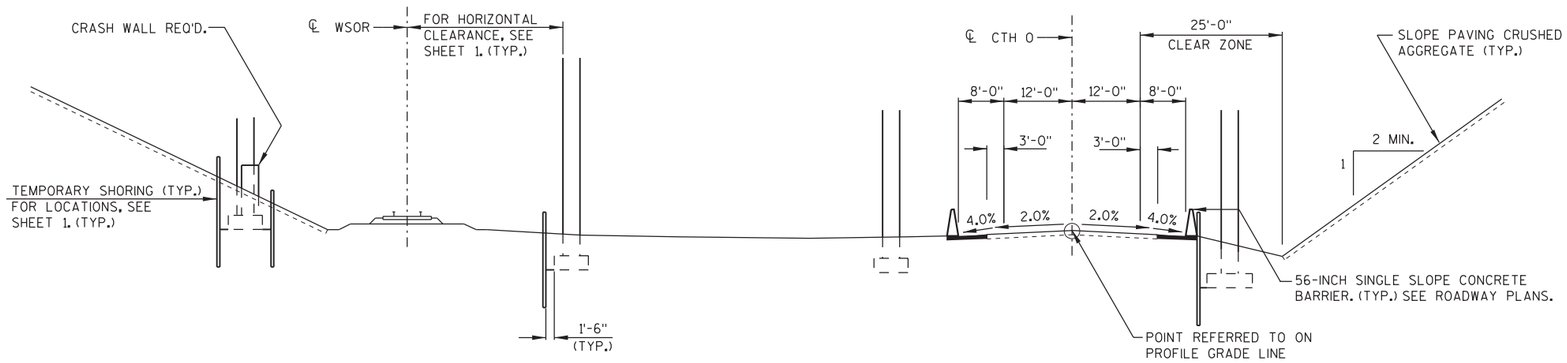
CROSS SECTIONS AND PROFILES

SHEET 2 OF 7

8

8

PRINTER DRIVER: S:\..acm-CADstds.Libraries\WISDOT\MicroStation\Resources\MS_Printing\Printer_Drivers\HE_PDF.11 x 17.plt
PEN TABLE: S:\..acm-CADstds.Libraries\WISDOT\MicroStation\Resources\MS_Printing\pen tables\HE_WisDOT.tbl
FILE NAME: L:\work\projects\6022267\400.Technical\408.Structural\408.1.Structure.Design\408.1.220 B-53-230_CTH O NB\408.1.220.12 Dgns\03.quants.b-53-230.dgn
PLOT DATE: 11/14/2014 PLOT TIME: 7:50:14 AM BATCH PRINT SHEET 3 OF 7



TYPICAL SECTION THRU CTH O & WSOR

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH SLOPE PAVING CRUSHED AGGREGATE TO THE EXTENT SHOWN ON SHEET 1, THE ABUTMENT DETAILS AND SLOPE PAVING DETAIL SHEET.

PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE TOP OF DECK & APPROACH SLAB AND THE FRONT FACE AND THE TOP OF THE PARAPET.

THE FIRST DIGIT OF A THREE DIGIT BAR MARK AND THE FIRST TWO DIGITS OF A FOUR DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

THE EXISTING STRUCTURE (B-53-230) IS A 5 SPAN PRESTRESSED CONCRETE GIRDER BRIDGE, 281.8' LONG x 59'± WIDE. THE SUBSTRUCTURE AND SUPER-STRUCTURE SHALL BE WIDENED BY ADDING 3 GIRDER LINES TO THE OUTSIDE SHOULDER AND ADDING 1 GIRDER LINE TO THE INSIDE SHOULDER SIDE OF THE BRIDGE.

ALL STATIONS AND ELEVATIONS ARE IN FEET.

OMIT PER WBM 6.3.2.1.2

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL PLANS, ~~THEREFORE, PRIOR TO THERE ACCEPTANCE THE CONTRACTOR SHALL FIELD VERIFY.~~

ALL CONCRETE REMOVAL SHALL BE DEFINED BY A 1" DEEP SAW CUT.

CLEAN, STRAIGHTEN, AND EXTEND EXISTING BAR STEEL REINFORCEMENT 24 BAR DIAMETERS INTO NEW CONSTRUCTION WHERE APPLICABLE.

ROUGHEN SURFACE OF CONCRETE 1/4" MIN. DEEP AT ALL AREAS OF NEW TO EXISTING CONCRETE CONTACT AT ABUTMENTS AND PIERS.

THE EXISTING ABUTMENTS AND PIERS TO REMAIN IN PLACE AS SHOWN AND INCORPORATED INTO NEW CONSTRUCTION.

THE PROPOSED GRADE SEPARATION PROJECT SHALL NOT INCREASE THE QUANTITY AND/OR CHARACTERISTICS OF THE FLOW IN THE RAILROAD'S EXISTING DITCHES AND/OR DRAINAGE STRUCTURES.

AT THE BACKFACE OF ABUTMENTS ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL.

THE EXISTING GROUNDLINE SHALL BE THE UPPER LIMITS OF EXCAVATION AT THE PIERS.

ELASTOMERIC BEARING PADS NEED NOT BE INDIVIDUALLY MOLDED PROVIDED THE CUT EDGES ARE SMOOTH AND TRUE.

THE HAUNCH CONCRETE QUANTITY IS BASED ON THE AVERAGE HAUNCH SHOWN ON THE PRESTRESSED GIRDER DETAILS SHEET.

CLARIFY WHICH COMPONENTS TO BE HPC.

ADD FOLLOWING NOTES FROM 6.3.2.1.2:
-VARIATIONS TO NEW GRADE LINE.....
-THE CONTRACTOR SHALL SUPPLY A NEW NAME PLATE.....

305.0120 BASE AGGREGATE DENSE 1 1/4- INCH TON

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	STA. BID ITEM	UNIT	SOUTH APPROACH	SOUTH ABUTMENT	PIER 1	PIER 2	PIER 3	PIER 4	NORTH ABUTMENT	NORTH APPROACH	SUPER.	TOTALS
203.0200	REMOVING OLD STRUCTURE 736+50	LS										1
203.0225.S	DEBRIS CONTAINMENT ~~STRUCTURE~~ B-53-230	LS										1
206.1000	EXCAVATION FOR STRUCTURES BRIDGES B-53-230	LS										1
210.0100	BACKFILL STRUCTURE	CY										
502.0100	CONCRETE MASONRY BRIDGES	CY										
502.3200	PROTECTIVE SURFACE TREATMENT	SY										
502.5005	MASONRY ANCHORS TYPE L NO. 5 BARS	EACH										
502.5020	MASONRY ANCHORS TYPE L NO. 8 BARS	EACH										
503.0136	PRESTRESSED GIRDER TYPE I 36-INCH	LF										
505.0405	BAR STEEL REINFORCEMENT HS BRIDGES	LB										
505.0605	BAR STEEL REINFORCEMENT HS COATED BRIDGES	LB										
506.2605	BEARING PADS ELASTOMERIC NON-LAMINATED	EACH										
506.4000	STEEL DIAPHRAGMS B-53-230	EACH										
509.1500	CONCRETE SURFACE REPAIR	SF										
511.1200	TEMPORARY SHORING B-53-230	SF										
514.0450	FLOOR DRAINS TYPE WF	EACH										
514.2608	DOWNSPOUT 8-INCH	LF										
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY										
550.0010	PRE-BORING UNCONSOLIDATED MATERIALS	LF										
550.2124	PILING CIP CONCRETE 12 3/4 X 0.25-INCH	LF										
604.0500	SLOPE PAVING CRUSHED AGGREGATE	SY										
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF										
614.0150	ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD	EACH										
652.0125	CONDUIT RIGID METALLIC 2-INCH	LF										
652.0225	CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH	LF										
653.0222	JUNCTION BOXES 18X12X6-INCH	EACH										
SPV.0035	HPC MASONRY STRUCTURES	CY										
SPV.0165	LONGITUDINAL DECK GROOVING	SF										
SPV.0085	BAR STEEL REINFORCEMENT HS STAINLESS BRIDGES	LB										
	NON-BID ITEMS											
	FILLER	SIZE										1/2" & 3/4"

INCLUDE ARCH. SURFACE TREATMENT AND STAINING?

PER WBM STD. 38.01, SPV REQUIRED FOR TEMPORARY SHORING ADJACENT TO RAILROAD

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-53-230

DRAWN BY TAW | PLANS CK'D. TR

GENERAL NOTES & QUANTITIES

SHEET 3 OF 7

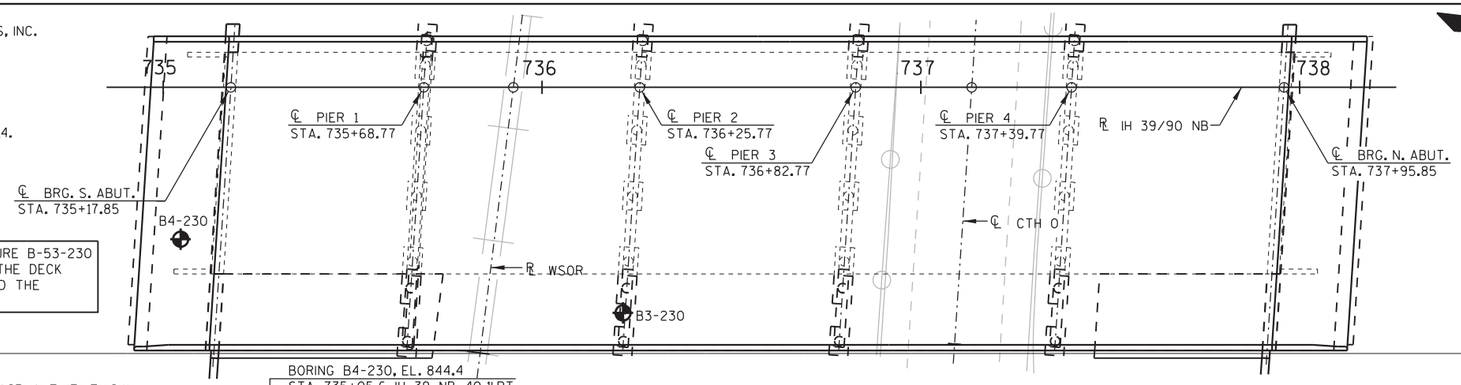
8

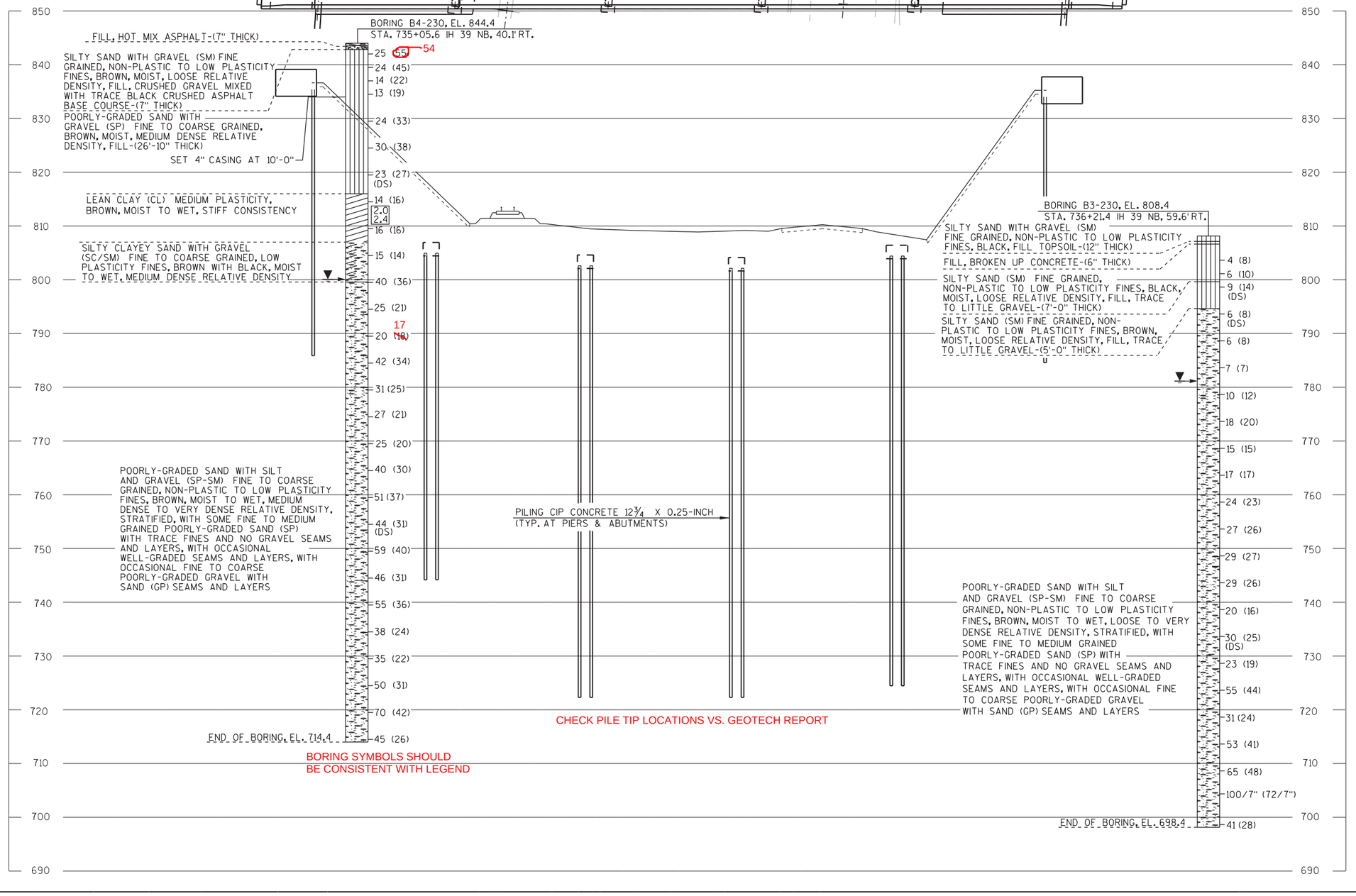
8

BORINGS AND REPORT BY:
SOILS AND ENGINEERING SERVICES, INC.
1102 STEWART STREET
MADISON, WI 53713-4648

BORING B3-230 WAS
PERFORMED APRIL 28, 2014.
BORING B4-230 WAS
PERFORMED ON JUNE 5 & 8, 2014.

THE EXISTING STRUCTURE B-53-230 TO REMAIN IN PLACE. THE DECK WILL BE REPLACED AND THE STRUCTURE WIDENED.





STATE PROJECT NUMBER

1003-10-84

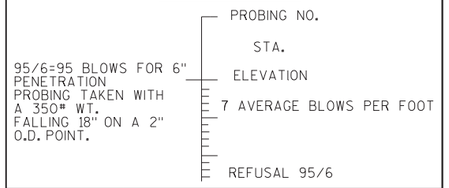
ABBREVIATIONS

F — FINE M — MEDIUM C — COARSE
WS — WEATHERED SO — SOUND

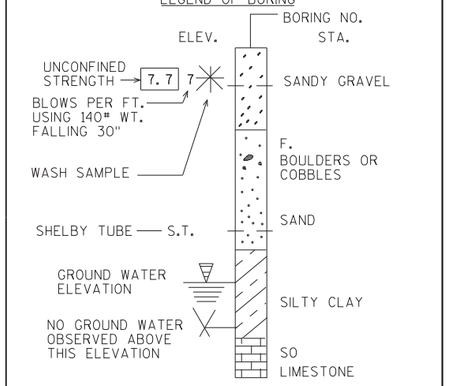
MATERIAL SYMBOLS

TOPSOIL, SILT, SANDSTONE, SAND, PEAT, LIMESTONE, GRAVEL, CLAY, IGNEOUS ROCK

LEGEND OF PROBING



LEGEND OF BORING



UNLESS OTHERWISE SPECIFIED, THE BLOWS PER FOOT AT THE LOCATIONS INDICATED ARE BASED ON DRIVING A 2" O.D. X 1.4" I.D. SPLIT SPOON SAMPLER WITH A 140# HAMMER HAVING A FREE FALL OF 30". THE BLOW COUNT IS TAKEN IN UNDISTURBED SOIL IMMEDIATELY BELOW A CASED OR OPEN HOLE ELIMINATING SIDE FRICTION ON THE DRIVE PIPE.

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

TO OBTAIN RELATIVE DATA CONCERNING THE CHARACTER OF MATERIAL IN AND UPON WHICH THE FOUNDATION MIGHT BE BUILT, BORINGS AND/OR SOUNDINGS WERE MADE AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING. THE DATA PRESENTED HEREIN REPRESENTS THE FINDINGS OF THE SUBSURFACE EXPLORATIONS MADE. HOWEVER, BECAUSE THE DEPTHS INVESTIGATED ARE LIMITED AND THE AREA OF THE BORINGS AND/OR SOUNDINGS IS VERY SMALL IN RELATION TO THE ENTIRE AREA, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT CONDITIONS BELOW THE DEPTHS INVESTIGATED OR THAT THE CLASSIFICATION OF MATERIAL ENCOUNTERED IN THESE INVESTIGATIONS IS NECESSARILY TYPICAL OF THE ENTIRE SITE.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-53-230

DRAWN BY T.A.W. | PLANS CK'D. TR

SUBSURFACE EXPLORATION

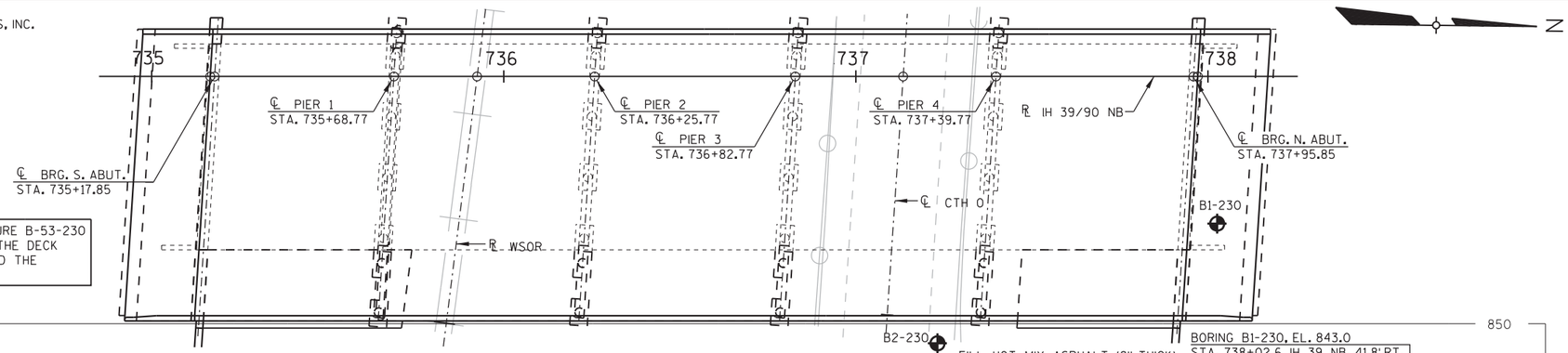
SHEET 4 OF 7

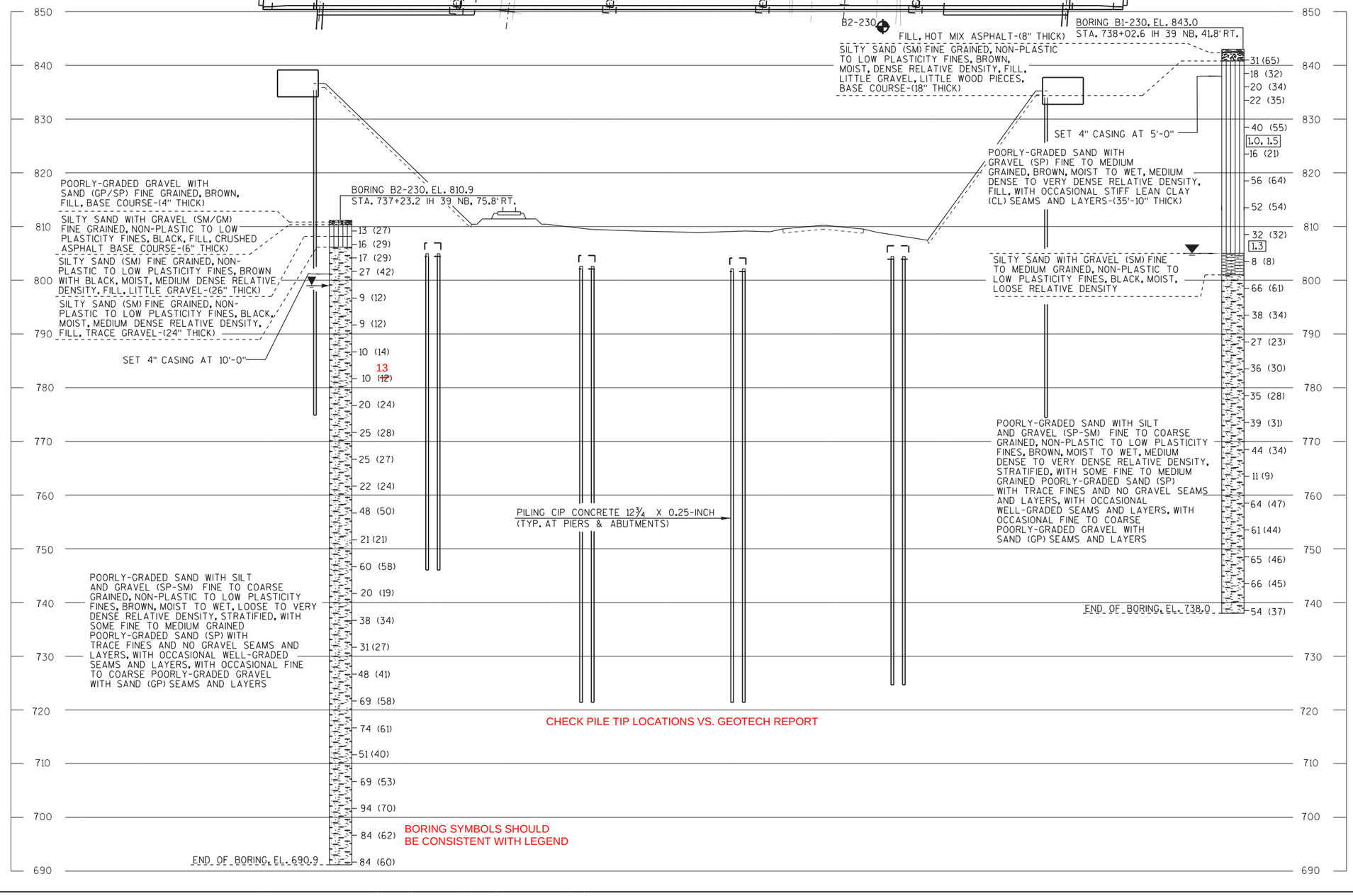
8

PRINTER DRIVER: S:\.acm-CADstds.Libraries\WISDOT\MicroStation\Resources\WS_Printing\Printer_Drivers\HE_PDF.11 x 17.plt
PEN TABLE: S:\.acm-CADstds.Libraries\WISDOT\MicroStation\Resources\WS_Printing\pen tables\HE_WsDOT.tbl
FILE NAME: L:\work\projects\60242867\400_Technical\408_Structural\408.1_Structure_Design\408.1.220 B-53-230_CTH O NB\408.1.220.12 Dgns\04_soils_b-53-230.dgn
PLOT DATE: 11/14/2014 PLOT TIME: 7:58:17 AM BATCH PRINT SHEET 4 OF 7
FILE = SCALE =

BORINGS AND REPORT BY:
SOILS AND ENGINEERING SERVICES, INC.
1102 STEWART STREET
MADISON, WI 53713-4648

BORING B1-230 WAS
PERFORMED JUNE 9 & 10, 2014.
BORING B2-230 WAS
PERFORMED ON MAY 6, 2014.





STATE PROJECT NUMBER

1003-10-84

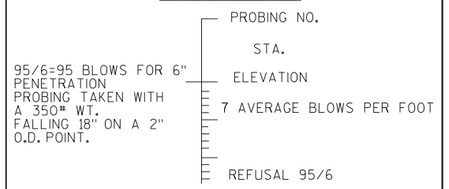
ABBREVIATIONS

F — FINE M — MEDIUM C — COARSE
WS — WEATHERED SO — SOUND

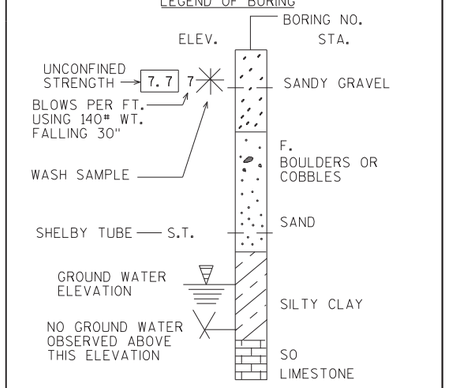
MATERIAL SYMBOLS

TOPSOIL, SILT, SANDSTONE
SAND, PEAT, LIMESTONE
GRAVEL, CLAY, IGNEOUS ROCK

LEGEND OF PROBING



LEGEND OF BORING



UNLESS OTHERWISE SPECIFIED, THE BLOWS PER FOOT AT THE LOCATIONS INDICATED ARE BASED ON DRIVING A 2" O.D. X 1.4" I.D. SPLIT SPOON SAMPLER WITH A 140# HAMMER HAVING A FREE FALL OF 30". THE BLOW COUNT IS TAKEN IN UNDISTURBED SOIL IMMEDIATELY BELOW A CASED OR OPEN HOLE ELIMINATING SIDE FRICTION ON THE DRIVE PIPE.

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

TO OBTAIN RELATIVE DATA CONCERNING THE CHARACTER OF MATERIAL IN AND UPON WHICH THE FOUNDATION MIGHT BE BUILT, BORINGS AND/OR SOUNDINGS WERE MADE AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING. THE DATA PRESENTED HEREIN REPRESENTS THE FINDINGS OF THE SUBSURFACE EXPLORATIONS MADE. HOWEVER, BECAUSE THE DEPTHS INVESTIGATED ARE LIMITED AND THE AREA OF THE BORINGS AND/OR SOUNDINGS IS VERY SMALL IN RELATION TO THE ENTIRE AREA, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT CONDITIONS BELOW THE DEPTHS INVESTIGATED OR THAT THE CLASSIFICATION OF MATERIAL ENCOUNTERED IN THESE INVESTIGATIONS IS NECESSARILY TYPICAL OF THE ENTIRE SITE.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-53-230

DRAWN BY T.A.W. | PLANS CK'D. TR

SUBSURFACE EXPLORATION

SHEET 5 OF 7

FILE =
SCALE =

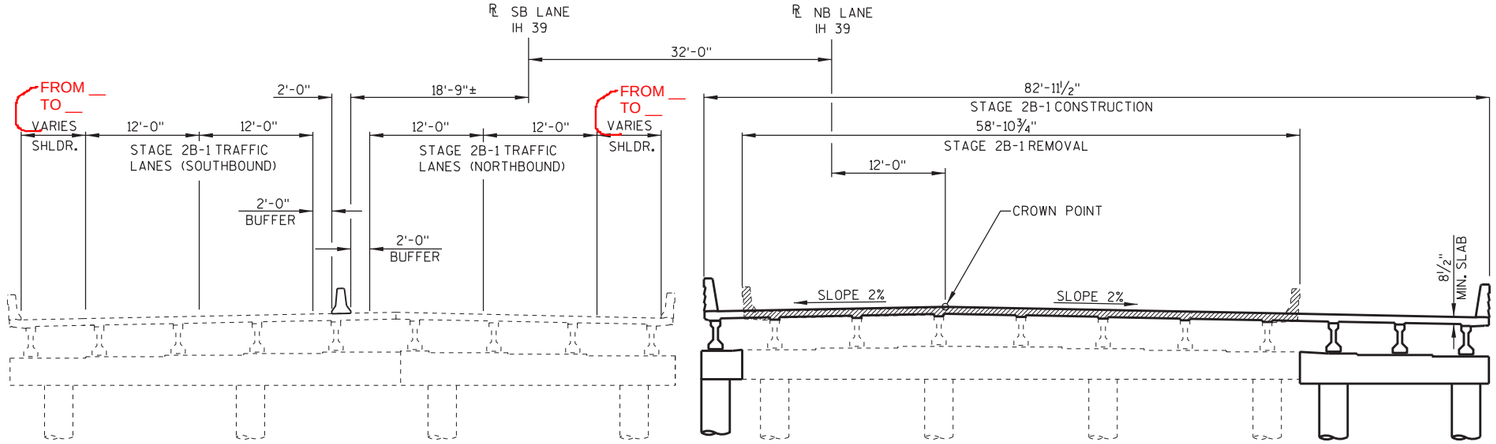
8

8

PRINTER DRIVER: S:_acm-CADstds_Libraries\WISDOT\MicroStation\Printing\Printer_Drivers\HE_PDF_11 x 17.plt
PEN TABLE: S:_acm-CADstds_Libraries\WISDOT\MicroStation\Resources\MS_Printing\pen tables\HE_WisDOT.tbl
FILE NAME: L:\work\projects\60242867\400 Technical\408 Structural\408.1 Structure Design\408.1.220 B-53-230_CTH O NB\408.1.220.12 Dgns\05 soils b-53-230.dgn
PLOT DATE: 11/14/2014 PLOT TIME: 7:58:19 AM BATCH PRINT SHEET 5 OF 7

PRINTER DRIVER: S:_acm-CADstds_Libraries\WISDOT\MicroStation\Resources\MS_Printing\Printer_Drivers\AE_PDF.11 x 17.plt
PEN TABLE: S:_acm-CADstds_Libraries\WISDOT\MicroStation\Resources\MS_Printing\pen tables\AE_WisDOT.tbl
FILE NAME: L:\work\projects\60242867\400 Technical\408 Structural\408.1 Structure Design\408.1.220 B-53-230_CTH O NB\408.1.220.12 Dgns\06 staging\b-53-230.dgn
PLOT DATE: 11/14/2014 PLOT TIME: 7:50:21 AM
BATCH PRINT SHEET 6 OF 7

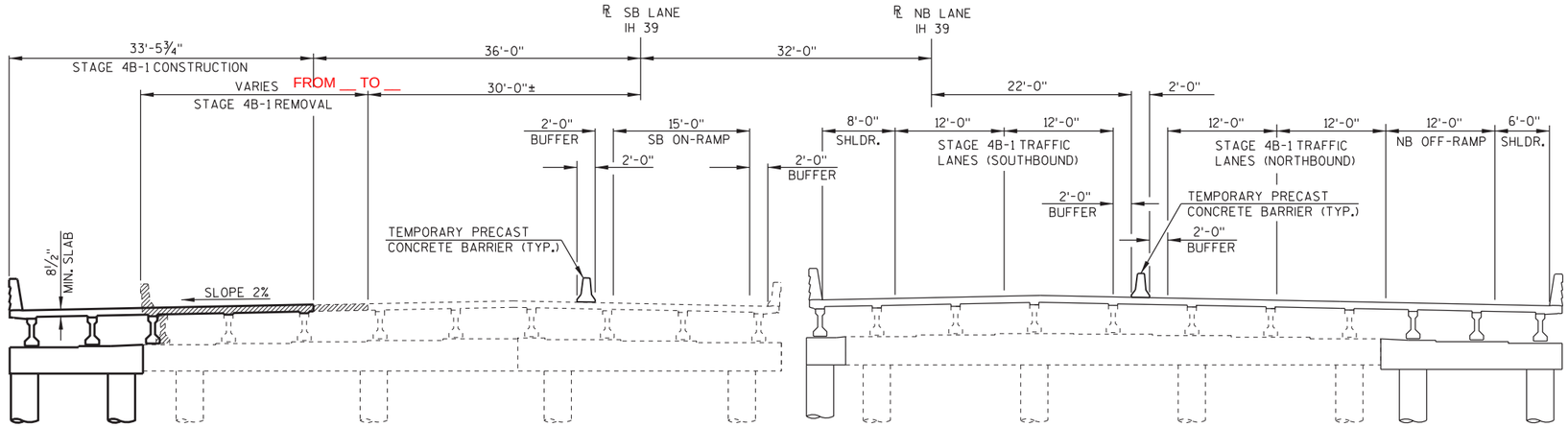
STATE PROJECT NUMBER
1003-10-84



B-53-229 B-53-230

STAGE 2B-1 - REMOVAL & CONSTRUCTION

(LOOKING NORTH)



B-53-229 B-53-230

STAGE 4B-1 - REMOVAL & CONSTRUCTION

(LOOKING NORTH)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-53-230

DRAWN BY TAW | PLANS CK'D. TR

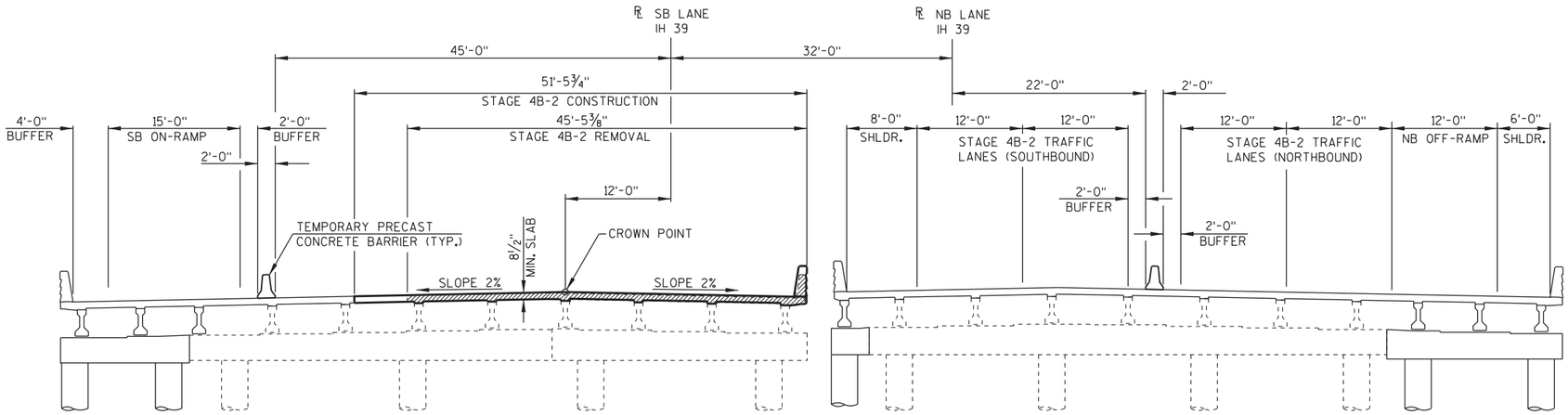
CONSTRUCTION STAGING

SHEET 6 OF 7

8

8

STATE PROJECT NUMBER
1003-10-84



B-53-229

B-53-230

STAGE 4B-2 - REMOVAL & CONSTRUCTION

(LOOKING NORTH)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-53-230

DRAWN BY TAW | PLANS CK'D. TR

CONSTRUCTION STAGING

SHEET 7 OF 7

8

8