STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Inspection Report for B-64-064

IH 43 NB over USH 12

Apr 26,2017

Type	Prior	Frequency (mos)	Performed
Routine	04-30-15	24	X
Deck Evaluation		0	X
SIA Review	04-30-15	48	
Vertical Clearance Measured	11-11-16	0	X

Latitude	42°40'25.95"N	Owner	STATE HIGHWAY DEPT
Longitude	88°31'19.39"W	Maintainer	STATE HIGHWAY DEPT

Time Log

Hours	Minutes
4	0

Team members

	Name	Number	Signature	Date
Inspector	Jashinsky, Dan	2010	Dan Jashinsky E-signed by Dan P Jashinsky(dotdpj)	12-06-17

BRIDGE INSPECTION REPORT
Wisconsin Department of Transportation
DT2007 2003 s.84.17 Wis. Stats.

Identification & Location

Feature On:	Section Town Range:	Structure Number:
IH 43 NB	S31 T03N R17E	B-64-064
Feature Under:	County:	
USH 12	WALWORTH	
Location	Municipality:	Structure Name:
1.9M N JCT STH 67 TO N	LA FAYETTE	

Geometry

measurements in feet, except where noted

Approach Roadway Width:	Bridge Roadway Width:	Total Length:
51	51.0	340.5
Approach Pavement Width:	Deck Width:	Deck Area (sq ft):
26	54.0	18387

Traffic

	Lanes	ADT	ADT year	Traffic Pattern
On	3	11550	2013	ONE WAY TRAFFIC
Under	8	15520	2013	TWO WAY TRAFFIC

Capacity

Inventory rating:	Overburden depth (in):	Last rating date:	Controlling:
HS18	2.0		INTERIOR DECK GIRDER Moment
Operating rating:	Deck surface material:	Re-rate for capacity (Y/N):	Control location:
HS30	LOW SLUMP CONCRETE		4.9 SPAN 3, 44.3
Posting:	Re-rate notes:		

Load Rating

Hydraulic

Scour Critical Code(113):	Q100 (ft3/sec):
(N) NO WATERWAY	0
High water elevation (ft):	Velocity (ft/sec):
0.0	0.0

Classification

Sufficiency #:

Span(s)

Span #	Material	Configuration	Depth (in)	Length (ft)	Main
1	CONT STEEL	DECK GIRDER		73.0	
2	CONT STEEL	DECK GIRDER		90.8	Y
3	CONT STEEL	DECK GIRDER		90.8	
4	CONT STEEL	DECK GIRDER		81.5	

Expansion joint(s)

Temperature: File: New: 70

Joint #	Location	Type	Last inspection date	Last measure (in)	New measure (in)
1	EAST ABUTMENT	STRIPSEAL	04-30-15	1.4	1.4
2	WEST ABUTMENT	STRIPSEAL	04-30-15	1.4	1.4

Clearance

Item	File Measurement (ft)	File Date	New Measurement (ft)
Highway Min Vertical Under Cardinal	14.95	11-Nov-2016	14.94
Highway Min Vertical Under Non-Cardinal	16.34	11-Nov-2016	
Horizontal Under Cardinal	69.1		
Horizontal Under Non-Cardinal	68.9		
Highway Min Vertical On Cardinal			
Horizontal On Cardinal			

Construction History

Year	Work Performed	FOS id
9999	NOT BUILT	1090-70-16
1988	PAINTING	1090-15-00
1985	OVERLAY - CONCRETE	0064-44-11
1969	NEW STRUCTURE	1081-02-79

Maintenance Items

Item	Priority	Recommended by	Status	Status change
Deck - Patching	HIGH	Brooks, Julie (2017)	IDENTIFIED	01/14/16
Patch spalls and repair/replace failing patches on surface.				
Drainage - Repair/Replace Deck Drains	HIGH	Brooks, Julie (2017)	IDENTIFIED	01/14/16
Repair inlet at SW wing corner: Tuckpoint inlet.				
IMP-Deck Replacement	MEDIUM	Brooks, Julie (2017)	IDENTIFIED	01/14/16
2020 recommended.				
IMP-Paint Structure	MEDIUM	Brooks, Julie (2017)	IDENTIFIED	01/14/16
2020 recommended.				
Misc - Remove/Monitor Loose Concrete	MEDIUM	Brooks, Julie (2017)	IDENTIFIED	01/14/16
Monitor any delams over roadway.				
Approach - Seal Cracks	MEDIUM	Brooks, Julie (2017)	IDENTIFIED	01/14/16
Seal all cracks and joints in approaches.				

Elements

Chk	Element	Defect	Description	UOM	Total	Quantity in Condition State 1	2	3	4
X	**12**		**Reinforced Concrete Deck**	SF	18,387	17,848	464	75	0
		1080	Delamination - Spall - Patched Area **S**palls with exp. rebar, delams**.**	SF		0	9	15	0
		1130	Cracking (RC) **Nrw-Med trans and map** cracking with effl.	SF		0	455	60	0
	8514		Concrete Overlay	SF	17,366	10,135	7,057	174	0
		3210	Debonding/Spall/Patched Area/Pothole **2005 IR Indicated 10% - 15 % Delamination; 2010 IR Indicated 10-15% Delamination; 2015 IR Indicated 15-20% delamination.** Many patched areas - mostly along deflection joints - many failing; many hollow-sounding areas**.**	SF		0	3,583	156	0
		3220	Crack (Wearing Surface) **HL-Med** Trans, Diag, Longit, & Lg-Pattern map cracking**.** **CS 1 est at 10%** **CS 2 est at 20%**	SF		1,737	3,474	18	0
X	**107**		**Steel Open Girder** **Girders numbered from north to south.**	LF	1,704	0	533	1,171	0
		1000	Corrosion Laminate rust at ends; mod-hvy rust on bottom flanges over roadways, lt rust on webs**, moderate edge rust.**	LF		0	533	1,171	0
	8516		Painted Steel **Painted 1988.**	SF	23,429	0	15,932	5,857	1,640
		3440	Effectiveness (Steel Protective Coatings) Paint no longer effective in areas of rust; peeling paint on btm flgs. at ends, piers and over roadways; Fading and chalking throughout**.** **CS 3 est at 25%; CS 4 est at 7%**	SF		0	15,932	5,857	1,640

X	**205**		**Reinforced Concrete Column**	EA	15	13	2	0	0
			Few pop-outs on all columns						
		1080	Delamination - Spall - Patched Area	EA		0	1	0	0
			Pier 2, C5 - shallow delams						
		1130	Cracking (RC)	EA		0	1	0	0
			Pier 1, C5 - Hl crack w/ rust stains						
X	**215**		**Reinforced Concrete Abutment**	LF	120	98	13	9	0
		1080	Delamination - Spall - Patched Area	LF		0	5	4	0
			E. abut: Med delams**,** spalling w/ exp rebar @ NE & SE**.** **W. abut: Lg** spall **w/exp rebar** lower body @ S**W** corner**, Lg vert spall.**						
		1130	Cracking (RC)	LF		12	8	5	0
			HL-Med cracking w/ leaching Vert & Diag cracks in lower body, map cracking at backwall ends**.**						
X	**300**		**Strip Seal Expansion Joint**	LF	111	12	73	26	0
			Dirt in seals. **W** Jt: 1-3/8" @ **70** deg @ Lt Shldr; **E** Jt: 1-3/8" @ **70** deg @ Lt Shldr; , HL cracks normal to Jt.						
		2310	Leakage, Seal Adhesion, Damage,Cracking	LF		0	49	0	0
			Seepage and leaking from joints**.**						
		2350	Debris Impaction	LF		0	24	0	0
			Some dirt in seals**.**						
		2360	Adjacent Deck or Header Damage	LF		0	0	26	0
			W. joint: Spalls in PB patched with asphalt, Lg spall/delam along S extrusion in Ln 3 in deck, Spall in deck next to extrusion in Ln 2**.** **E.** joint: Sm-med spalls in PB - some filled w/ asphalt or tar, Lg spall (failing patch) next to extrusion in deck at CL/Ln 2**.**						
X	**311**		**Moveable Bearing**	EA	20	0	11	8	1
		1000	Corrosion	EA		0	11	8	1
			Mod-Hvy rust @ **E & W** abuts, Lt-Mod rust at piers**.**						
		2210	Movement	EA		0	0	0	0
			Laminate rust impacting movement at south abutment						
X	**313**		**Fixed Bearing**	EA	5	0	4	1	0
			Center Pier						
		1000	Corrosion	EA		0	4	1	0
			Lt - Med rust on all fixed bearings**.**						
X	**331**		**Reinforced Concrete Bridge Rail**	LF	757	93	307	357	0
		1080	Delamination - Spall - Patched Area	LF		0	179	177	0
			Sm-Lg delams & spalls w/ exp rebar**.**						
		1130	Cracking (RC)	LF		22	128	180	0
			Hl-Med Horiz, Longit, Vert & Map cracking**.**						
X	**8400**		**Integral Wingwall**	EA	4	1	3	0	0
		8902	Wall Movement	EA		0	2	0	0
			S**E** wing out 5/8" **(same 2017); SW** wing out 1/2" **(same 2017).**						
		8903	Wall Deterioration	EA		3	1	0	0
			NE Wing: HL Diag & map cracking w/ effl; **S**E Wing: Diag & map cracking, shallow spalls & delams; S**W** Wing: Diag, Horiz & map cracking w/ effl.; **N**W Wing: HL cracking**.**						

BRIDGE INSPECTION REPORT
Wisconsin Department of Transportation
DT2007 2003 s.84.17 Wis. Stats.

Structure No.:**B-64-064**

Assessments

Chk	Element	Defect	Description	UOM	Total	Quantity in Condition State 1	2	3	4
X	**9001**		**Drainage - Ends of Structure** **N**W Inlet: missing blocks and deteriorating mortar, erosion along wing, outlet pipe almost completely covered with debris @ asphalt; N**E**: minor erosion along wing; S**W**: Minor deterioration at top of inlet; **S**E: curb up to wing**.**	EA	4	1	2	1	0
X	**9030**		**Signs - Object Markers** 2 Tigers	EA	2	2	0	0	0
X	**9043**		**Slope Protection- Crushed Aggregate with Bit.** Slopes settled on top (w Slope dropped about 2-ft), but stable; light vegetation; bleaching on both	EA	2	0	2	0	0
X	**9167**		**Steel Diaphragm** Hvy rust @ W. Abut. Lt-mod rust on flanges, some chalking and blush rust on webs.	EA	80	0	39	41	0
X	**9323**		**Approach Roadway - Asphalt** **W.** Appr: repaved in 2014, minor cracking & distress in Ln 2 @ PB, asphalt rough and poorly consolidated along outside parapet**.** **E.** Appr: Recently repaved, **Lt shldr**, Ln 1**, Ln 2** sealed w/ tar @ PB & CL between lanes 1&2, Ln 3 has unsealed longit & trans cracks**.**	EA	2	2	0	0	0
X	**9335**		**Decorative Rail** Tubular rail on top of parapets, scrapes on median rail	EA	2	2	0	0	0

NBI Ratings

	File	New
Deck	5	5
Superstructure	5	5
Substructure	5	5
Culvert	N	N
Channel	N	N
Waterway	N	N

Structure Specific Notes

Cardinal Minimum Vertical Clearance (15.23', **4/26/17**) Measured at G5 (South Fascia) at Lane 1/2 Joint; Cardinal CD Ramp Minimum Vertical Clearance (14.9**4', 4/26/17**) Measured at G5 (South Fascia) at **Rt Edge Line; Non-Cardinal Minimum Vertical Clearance (16.34', 4/26/17) Measured at G5 (South Fascia) at Lt Edge Line; Non-Cardinal CD Ramp Minimum Vertical Clearance (17.52', 4/26/17) Measured at G5 (South Fascia) at Lt Edge Line.**

Inspection Specific Notes

I-43 is signed as a north/south roadway. For inspection purposes the bridge is referenced in an east/west direction per original plan set.

Inspector Site-Specific Safety Considerations

Structure Inspection Procedures

Special Requirements

Chk	Hours	Cost	Comments

Routine

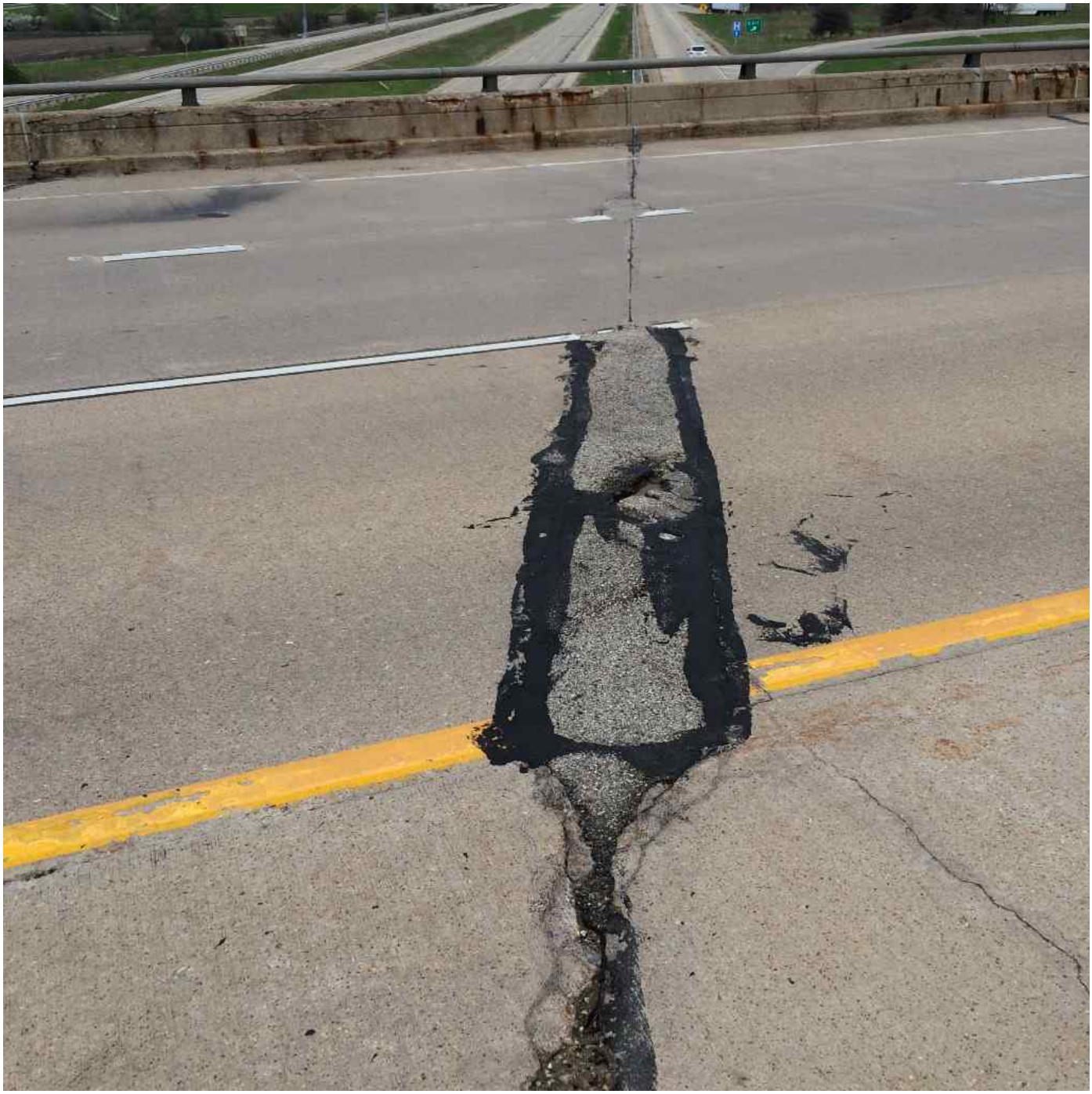
Document Comment/Description

Roadway looking north.

Routine

Document Comment/Description

Failing deck patch at Pier 1-Typical.



Routine

Document Comment/Description

Deck spall at right exit lane at Pier 3.

Routine

Document Comment/Description

Spall at SW corner deck soffit.

Routine

Document Comment/Description

Spall at SW corner south abutment.

Routine

Document Comment/Description

Bearing at girder 5 (SW) west abutment.

Routine

Document Comment/Description

Spall southeast corner backwall at east abutment.

Routine

Document Comment/Description

Spall with exposed rebar - North parapet, typical.
Photo copied from previous inspection - no change 2017.

Routine

Document Comment/Description

Medium crack and delams - South parapet, typical.
Photo copied from previous inspection - no change 2017.

Routine

Document Comment/Description

East abutment - Medium crack.
Photo copied from previous inspection - no change 2017.

Routine

Document Comment/Description

Girder 5 / Bearing at East abutment - Rust and bent keeper bar.
Photo copied from previous inspection - no change 2017.

Routine

Document Comment/Description

Diaphragm with laminate rust at West Abutment, typical.
Photo copied from previous inspection - no change 2017.

Routine

Document Comment/Description

Deck spall and delam - South soffit, near pier.
Photo copied from previous inspection - no change 2017.

Routine

Document Comment/Description

Typical girder rust and failing paint.
Photo copied from previous inspection - no change 2017.

Vertical Clearance Verification

Document Comment/Description

Minimum vertical clearance at eastbound CD ramp 14.94'

Vertical Clearance Verification

Document Comment/Description

Minimum vertical clearance at eastbound US12 15.23'

Vertical Clearance Verification

Document Comment/Description

Minimum vertical clearance at westbound US12 16.34'

Vertical Clearance Verification

Document Comment/Description

Minimum vertical clearance at westbound CD ramp 17.52'

Non-Image Documents

Type	Document	Document Comment/Description	Attached
Deck Evaluation	b64-064_17_Kd1.pdf	IR Evaluation 2015	X

DECK INSPECTION SHEET

STRUCTURE NO.: B-64-64

FEATURE ON	MAINTAINER	COUNTY	ROADWAY WIDTH (FEET)	TOTAL LENGTH (FEET)
IH 43 (NB)	State	Walworth	57.0	340.5
FEATURE UNDER	**LOCATION**	**SKEW ANGLE**	**DECK AREA (sq.ft.)**	**RDWY AREA (sq.ft.)**
Ramp IH43N to WSH RW	1.9 M N Jct STH 67 to N	25 Left	18,387	17,366
STRUCTURE TYPE	**SPANS**	**LENGTHS**	**NO. OF LANES**	**NO. OF SHOULDERS**
Cont Steel Deck Girder	4	73.0, 90.8, 90.8, 81.5	3	2

CONSTRUCTION HISTORY	YEAR	WORK PERFORMED
	1969	New Structure
	1985	Concrete Overlay

INFRARED SURVEY RESULTS (LEVEL 1)

ESTIMATED % TOTAL DISTRESS* 15-20 %

If <2% _____

DATE OF SURVEY	TOTAL ROADWAY AREA (sq. ft.)	AREA IN SHADE/DEBRIS (sq. ft.)	AREA INSPECTED (sq. ft.)
4/23/15	17,366	None	17,366

ESTIMATED % TOTAL DISTRESS IS THE DEFECT AREAS ONLY AND IS NOT THE ESTIMATED REHABILITATION AREA

TYPE OF DEFECT	PERCENT OF AREA INSPECTED					
	0-5	5-10	10-15	15-20	20-25	25+
Delamination				X		
Debonding	None					
Concrete Patching	< 2%					
Asphalt Patching	< 2%					
Spalling	< 2%					
PREVIOUS SURVEYS						
YEAR LEVEL (Total Defects)						
2010 1			X			
2005 1			X			
2001 1		X				
1999 1		X				
1998 1		X				

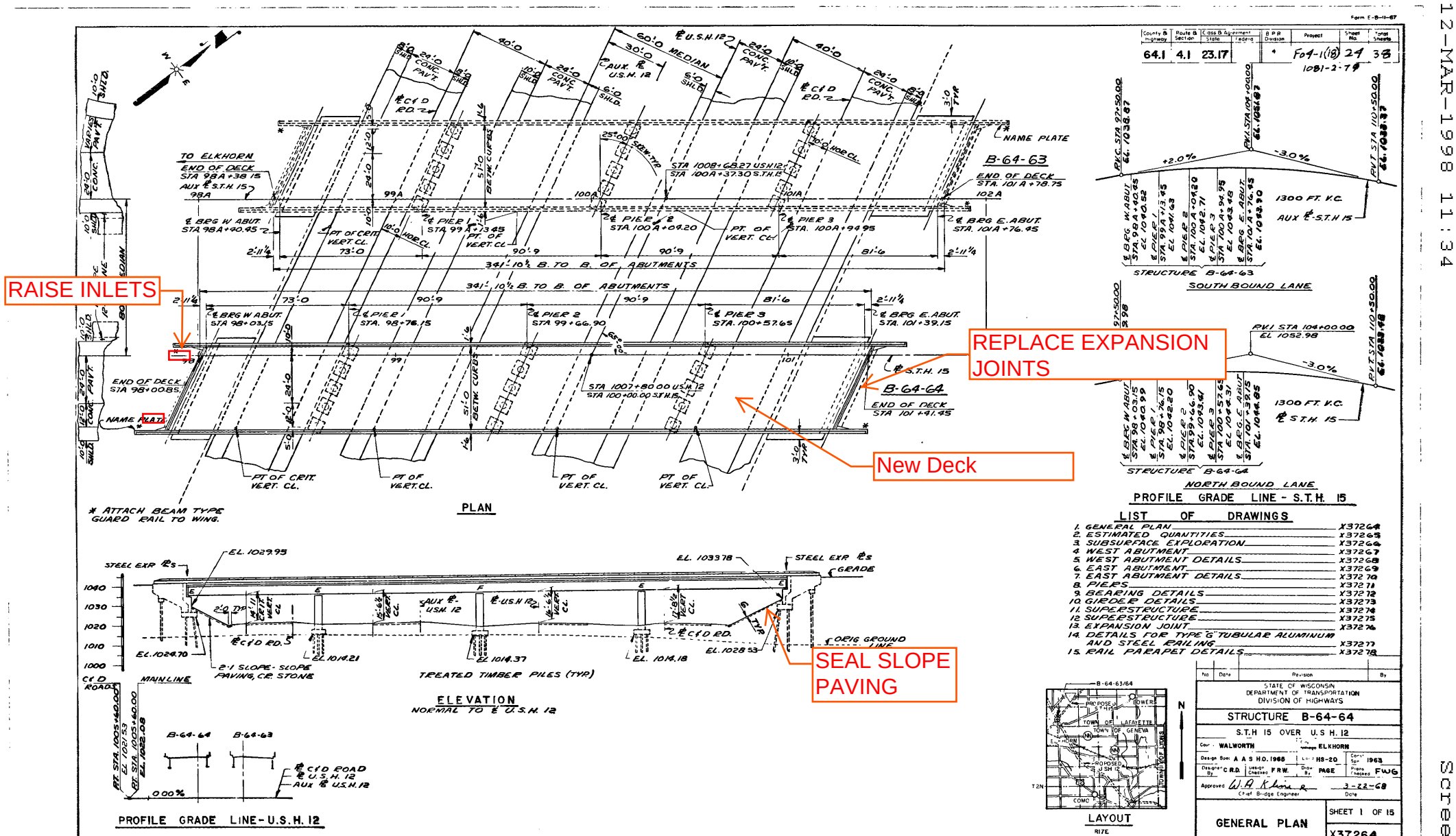
COMMENTS: Medium and large delaminations observed, scattered throughout the deck.
Most of the existing PCC patching is in poor condition.

 # PATCHES: 28 PCC (Approx. 225 ft² total), 1 AC (Approx. 3 ft²)

 # OF CORES: Ø **RESULTS:** _____

PROJECT ID.: 1000-70-15

WORK ORDER: #2



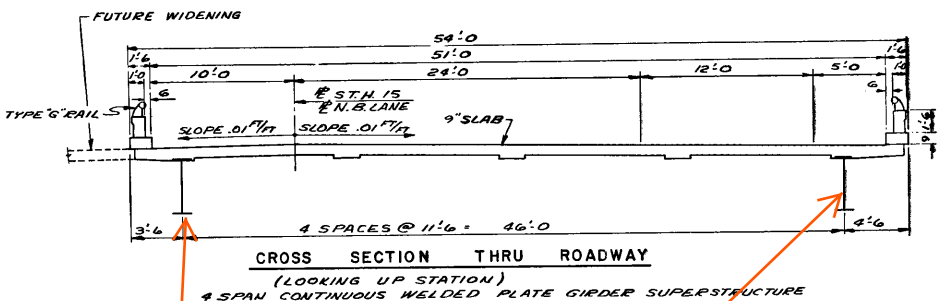
Form E-8-12-67

B.P.R. Division	Project	Sheet Number	Total Sheets
4	F04-1(18) 25 38		
	1081-2-79		

TOTAL ESTIMATED QUANTITIES

BID ITEMS	UNIT	W ABUT	PIER 1	PIER 2	PIER 3	E ABUT	SUPER	TOTAL
EXCAVATION FOR STRUCTURES	C.Y.	150	140	130	140	150		710
GRANULAR BACKFILL	C.Y.	230				230		460
CONCRETE MASONRY	C.Y.	134.0	65.7	67.2	68.5	134.5	576.8	1,046.7
BAR STEEL REINFORCEMENT	LB.	7,550	9,010	9,300	9,540	7,520	164,040	206,960
STRUCTURAL CARBON STEEL	LB.						218,800	218,800
STRUCTURAL LOW-ALLOY STEEL	LB.						156,810	156,810
LUBRICATED BRONZE PLATE	LB.						420	420
BEARING PADS	S.F.						40	40
* TREATED TIMBER TEST PILING	LUMP SUM							1
TREATED TIMBER PILING, DELIVERED	L.F.	1,300	1,365	1,365	1,365	1,300		6,695
TREATED TIMBER PILING, DRIVEN	L.F.	1,300	1,365	1,365	1,365	1,300		6,695
TUBULAR RAILING, TYPE 'S'	L.F.						748	748
SLOPE PAVING, CRUSHED STONE	S.Y.	170				240		410
PREBORING TREATED TIMBER PILING	L.F.	300				480		780
NON-BID ITEMS								
1/8" ALUMINUM OR ZINC PLATE	S.F.						40	40
POLYVINYL CHLORIDE WATERSTOP	L.F.	14				14		28

*-2-70'-0 & 3-45'-0 TEST PILES REQ'D. DRIVE ONE 70'-0 TEST PILE AT EACH ABUTMENT. DRIVE ONE 45'-0 TEST PILE AT EACH PIER.



CROSS SECTION THRU ROADWAY
(LOOKING UP STATION)
4 SPAN CONTINUOUS WELDED PLATE GIRDER SUPERSTRUCTURE

REPLACE BEARINGS

STEEL MEMBERS ARE BEGINNING TO CORRODE

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.
BAR STEEL REINFORCEMENT SHALL BE IMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.
BEVEL EXPOSED EDGES OF CONCRETE 1" UNLESS SHOWN OR NOTED OTHERWISE.
JOINT FILLER SHALL CONFORM TO A.A.S.H.O. DESIGNATION M153 OR M213.
HOT POURED ELASTIC TYPE JOINT SEALER SHALL CONFORM TO A.S.T.M. DESIGNATION D1190.
ALL FIELD CONNECTIONS SHALL BE MADE WITH 3/4" ϕ FRICTION TYPE HIGH-TENSILE STRENGTH BOLTS UNLESS SHOWN OR NOTED OTHERWISE.
THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH SLOPE PAVING, CRUSHED STONE, TO THE EXTENT AS SHOWN ON SHEET 1 AND IN THE ABUTMENT DETAILS.
THE UPPER LIMIT FOR "EXCAVATION FOR STRUCTURES" SHALL BE THE FINISHED GRADED SECTION AT THE PIERS AND AS SHOWN ON SHEETS 5 & 7 FOR THE ABUTMENTS.
AT ABUTMENTS, ALL SPACES EXCAVATED AND NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH GRANULAR BACKFILL. FOR PAY LIMITS SEE SHEETS 5 & 7.
THE EXCAVATION AT FRONT FACE OF ABUTMENT MAY BE BACKFILLED TO BOTTOM OF SLOPE PROTECTION IMMEDIATELY AFTER ABUTMENT FORMS ARE REMOVED.

DESIGN DATA

LIVE LOAD: HS-20
ALLOWABLE DESIGN STRESSES
CONCRETE MASONRY, GRADE "AA" SLAB — f_c = 1,200 P.S.I.
CONCRETE MASONRY, GRADE "AA" ALL OTHER — f_c = 1,400 P.S.I.
BAR STEEL REINFORCEMENT — f_s = 20,000 P.S.I.
STRUCTURAL CARBON STEEL — f_s = 20,000 P.S.I.
STRUCTURAL LOW-ALLOY STEEL
THICKNESS 3/4" OR LESS — f_s = 27,000 P.S.I.
OVER 3/4" TO 8" INCLUDING 1 1/2" THICK — f_s = 25,000 P.S.I.

FOUNDATION DATA
ABUTMENTS TO BE SUPPORTED ON TREATED TIMBER PILES DRIVEN TO A MINIMUM BEARING CAPACITY OF 24 TONS/PILE. ESTIMATED PILE LENGTH= 50'-0. PREBORE ABUTMENT PILING TO ORIGINAL GROUND LINE.
PIERS TO BE SUPPORTED ON TREATED TIMBER PILES DRIVEN TO A MINIMUM BEARING CAPACITY OF 24 TONS/PILE. ESTIMATED PILE LENGTH = 35'-0.

No.	Date	Revision	By

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

STRUCTURE B-64-64

Date Set	Drawn By	Plans Checked
1963	PAGE	FWG

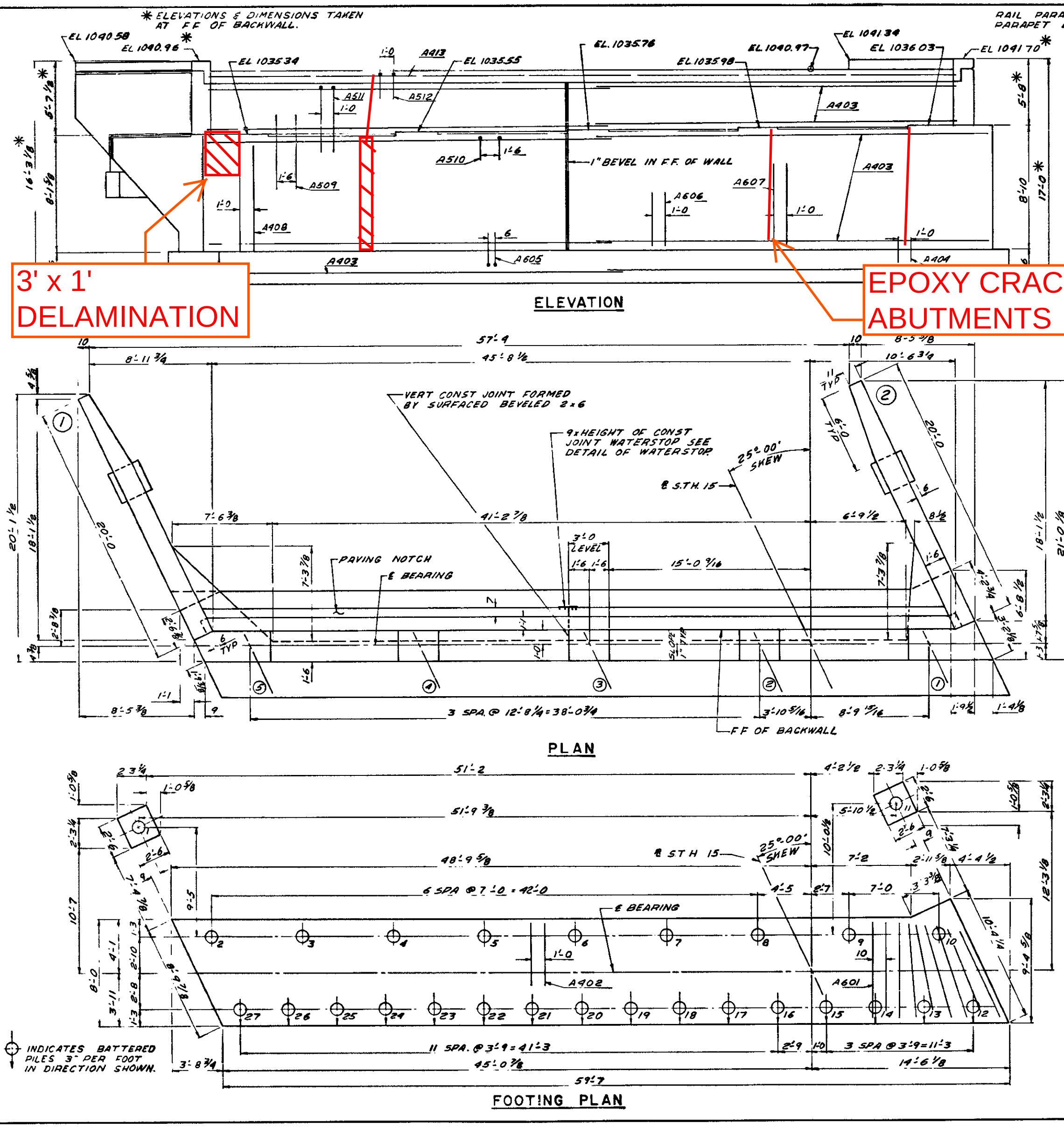
ESTIMATED QUANTITIES

SHEET 2 OF 15

X37265

Form E-B-12-67

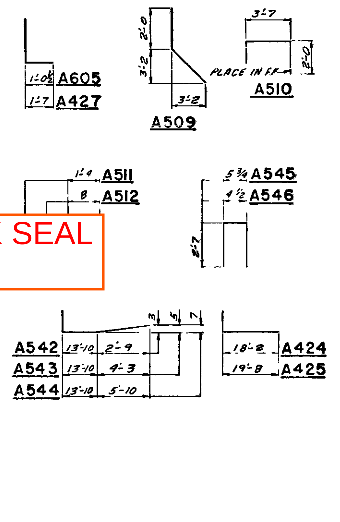
B P R Division	Project	Sheet Number	Total Sheets
4	F04-1(18)	27	39



ELEVATION

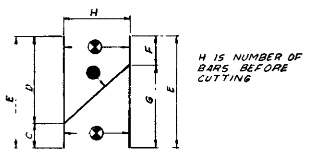
PLAN

FOOTING PLAN



BAR BEND DETAILS

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.
THE FIRST DIGIT OF A THREE DIGIT MARK SIGNIFIES THE BAR SIZE.
BAR BENDS SHALL CONFORM TO "RECOMMENDED HOOKS, ALL GRADES," AND RECOMMENDED SIZES FOR STIRRUP AND TIE HOOKS AS FOUND IN A C I STANDARD 315.



CUTTING DIAGRAM

BUNDLE AND MARK CUT BARS WITH BAR AND SET NUMBER. BENT BARS USED IN A CUTTING DIAGRAM SHALL BE BENT AFTER CUTTING.

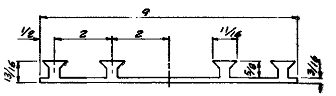
MARK AND CUT ALL BARS ALONG THIS LINE. MAKE ALL CUTS NORMAL TO BAR AXIS.

MARK		C	D	E	F	G	H	SETS REQ'D
A420	SET 1	3-9		14-7		10-10	11	1
	SET 1		10-10		3-9			1
A423	SET 2	4-11		23-7		17-8	5	1
	SET 3		18-8		5-11			1
A434	SET 4	3-9		13-2		9-5	9	1
	SET 4		9-5		3-9			1
A638	SET 5	7-9		18-0		13-3	4	1
	SET 6		10-3		4-9			1
A641	SET 7	5-3		15-6		11-6	5	1
	SET 8		10-3		4-0			1

BILL OF BARS

7,550 # 10 81-2-79

MARK	NO REQ'D	LENGTH	BENT	CUT DIAGR.	LOCATION
A601	72	7-6			FOOTING - BOT
A402	60	4-9			" - TOP
A403	56	30-1			" BODY BACKWALL - HORIZ
A404	60	2-0			" & BODY FF - DOWELS
A605	120	3-8	X		" " BF - DOWELS
A606	60	4-7			BODY BF - VERT
A607	60	6-0			" " "
A408	60	7-11			" FF "
A509	40	6-6	X		" - STIRRUPS
A510	40	6-7	X		" "
A511	58	11-5	X		BACKWALL - STIRRUPS
A512	58	4-7	X		PAVING BLOCK - STIRRUPS
A413	14	8-0			" " - HORIZ
A414	7	2-0			FOOTING & WING FF - DOWELS
A415	1	8-0			WING 1 FF - VERT.
A416	1	8-8			" 2 " "
A417	2	13-6			" 1 " "
A418	3	14-3			" 2 " "
A419	1	11-6			" 1 " "
A420	11	14-7		X	" 1&2 " " SET 1
A421	1	3-11			" 1 " HORIZ.
A422	1	7-0			" 2 " "
A423	5	23-7		X	" 1&2 " " SET 2&3
A424	2	19-1	X		" 1&2 " "
A425	6	20-7	X		" 1&2 " "
A426	4	2-2			WING FOOTING - HORIZ.
A427	8	5-6	X		" " - VERT.
A428	3	7-9			WING 1 B.F. - VERT FILLET
A429	3	8-8			" 2 " " "
A430	1	6-1			" 1 " " "
A431	1	5-3			" 1 " " "
A432	2	4-8			" 1&2 " " "
A433	1	5-4			" 2 " " "
A434	9	13-2		X	" 1&2 " " SET 4
A435	10	6-6			" 1&2 " "
A636	3	6-9			" 1 " - HORIZ FILLET
A637	3	4-11			" 2 " " "
A638	4	18-0		X	" 1&2 " " " SET 5&6
A639	5	13-6			" 1 " " "
A640	5	9-9			" 2 " " "
A641	5	15-6		X	" 1&2 " " " SET 7&8
A542	2	17-8	X		" 1&2 " "
A543	2	19-3	X		" 1&2 " "
A544	12	20-9	X		" 1&2 " "
A545	42	5-5	X		RAIL PARAPET - STIRRUPS WING 1&2
A546	8	5-4	X		" " " " 1&2
A547	8	17-4			" " - HORIZ. WING 1&2
A548	8	3-3			" " " " 1&2



POLYVINYL CHLORIDE WATERSTOP

No	Date	Revision	By

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

STRUCTURE B-64-64

Const. Spec. 1963 | Drawn By G L D | Plans Checked F W G

WEST ABUTMENT

SHEET 4 OF 15

X 37267

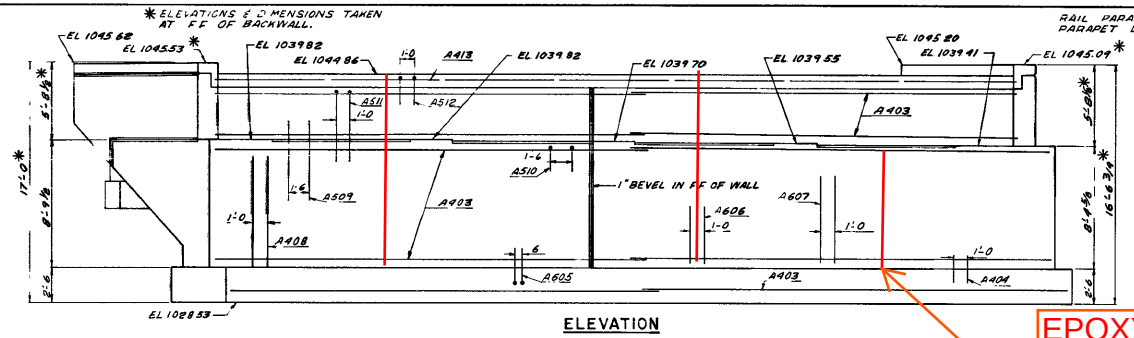
12-MAR-1998 11:36

ScreenScan

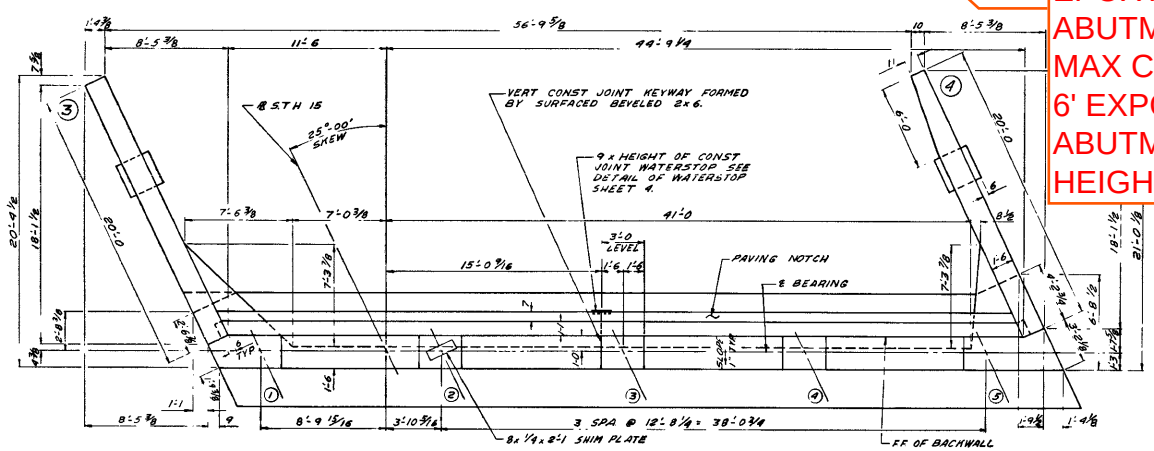
Form E-8-12-67

B P R Division	Project	Sheet Number	Total Sheets
4	F04-1(18)29	38	

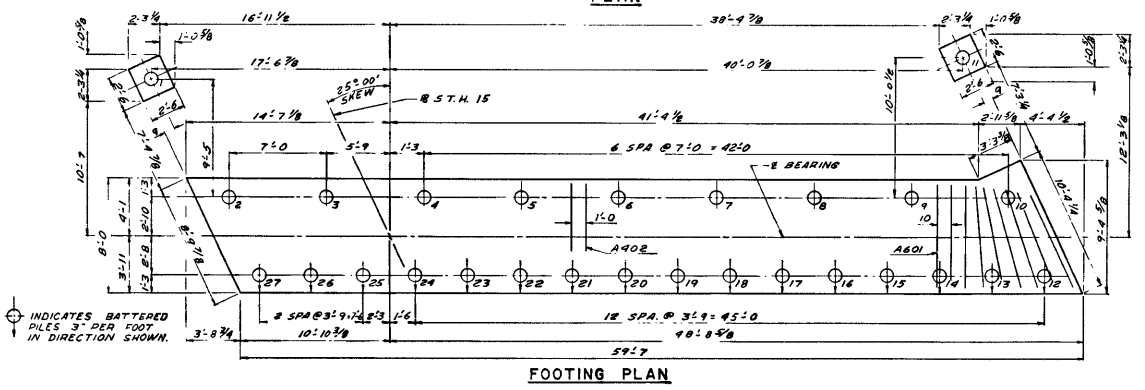
7,520 # 10-31-2-79



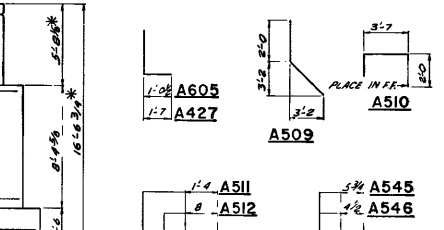
ELEVATION



PLAN



FOOTING PLAN

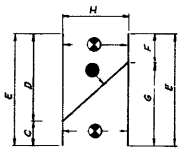


EPOXY CRACK SEAL ABUTMENTS. 0.060" MAX CRACK WIDTH. 6' EXPOSED ABUTMENT BODY HEIGHT

18'-5	A465
18'-3	A466
19'-8	A425
16'-10	A550
18'-4	A551
19'-8	A552

BAR BEND DETAILS

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.
THE FIRST DIGIT OF A THREE DIGIT MARK SIGNIFIES THE BAR SIZE.
BAR BENDS SHALL CONFORM TO "RECOMMENDED HOOKS, ALL GRADES" AND "RECOMMENDED SIZES FOR STIRRUP AND TIE HOOKS AS FOUND IN A C I STANDARD 315.



H IS NUMBER OF BARS BEFORE CUTTING

CUTTING DIAGRAM

- BUNDLE AND MARK CUT BARS WITH BAR AND SET NUMBER. BENT BARS USED IN A CUTTING DIAGRAM SHALL BE BENT AFTER CUTTING.
- MARK AND CUT ALL BARS ALONG THIS LINE. MAKE ALL CUTS NORMAL TO BAR AXIS.

BILL OF BARS

MARK	NO REQ'D	LENGTH	BENT	CUT DIAGR	LOCATION
A601	72	7-6			FOOTING - BOT
A402	60	4-9			" - TOP
A403	56	30-1			" BODY BACKWALL - HORIZ
A404	60	2-0			" & BODY FF - DOWELS
A603	120	3-8	X		" BF - DOWELS
A606	60	4-7			BODY BF - VERT
A607	60	6-0			" " "
A408	60	7-11			" FF "
A509	40	6-6	X		" - STIRRUPS
A510	40	6-7	X		" "
A511	58	11-5	X		BACKWALL - STIRRUPS
A512	58	4-7	X		PAVING BLOCK - STIRRUPS
A413	14	8-0			" " - HORIZ
A414	7	2-0			FOOTING & WING FF - DOWELS
A460	1	9-2			WING 4 FF - VERT.
A416	1	8-8			" 3 " "
A461	3	13-10			" 4 " "
A418	2	14-3			" 5 " "
A462	1	12-0			" 3 " "
A463	11	19-10	X		" 3&4 " " SET 1
A421	1	3-11			" 3 " HORIZ.
A422	1	7-0			" 4 " "
A464	5	23-10	X		" 3&4 " " SET 2&3
A465	1	19-4	X		" 3 " "
A466	1	19-3	X		" 4 " "
A425	6	20-7	X		" 3&4 " "
A426	4	8-2			WING FOOTING - HORIZ
A427	8	5-6	X		" " - VERT.
A467	3	8-2			WING 4 BF - VERT FILLET
A429	3	8-8			" 3 " " "
A468	1	6-7			" 3 " " "
A469	1	5-9			" 3 " " "
A470	1	4-11			" 3 " " "
A471	1	5-0			" 4 " " "
A472	1	9-4			" 4 " " "
A434	9	13-2	X		" 3&4 " " SET 4
A435	10	6-6			" 3&4 " "
A636	3	6-9			" 3 " HORIZ FILLET
A637	3	4-11			" 4 " " "
A673	4	17-9	X		" 3&4 " " " SET 5&6
A639	5	13-6			" 3 " " "
A640	5	9-9			" 4 " " "
A674	5	15-6	X		" 4 " " SET 7&8
A542	1	17-8	X		" 4 " "
A543	1	19-3	X		" 4 " "
A544	6	20-9	X		" 3&4 " "
A545	91	5-5	X		RAIL PARAPET - STIRRUPS WING 3&4
A546	4	5-9	X		" " " " 4
A547	4	17-4			" " - HORIZ. " 4
A548	4	3-3			" " " " 4
A549	4	19-4			" " " " 3
A550	1	18-0	X		WING 3 HORIZ - B.F.
A551	1	19-5	X		" 3 " "
A552	6	20-9	X		" 3 " "

MARK		C	D	E	F	G	H	SETS REQ'D
A463	SET 1	3-9		14-10		11-1	11	1
	SET 1		11-1		3-9			1
A464	SET 2	5-9		23-10		17-8	5	1
	SET 3		18-1		6-2			1
A434	SET 4	3-9		13-2		9-5	9	1
	SET 4		9-5		3-9			1
A673	SET 5	7-8		17-9		12-9	4	1
	SET 6		10-1		5-0			1
A674	SET 7	4-3		15-6		10-6	5	1
	SET 8		11-3		5-0			1

No.	Date	Revision	By

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

STRUCTURE B-64-64

Const Spec 1963 | Drawn By G L D | Plans Checked F W G

EAST ABUTMENT

SHEET 6 OF 15

X 37269

12-MAR-1998 11:38

B-64-63

Preliminary Load Rating Correspondence

From: Smith, Andrew D - DOT
To: Wood, Kevin
Cc: Dietsche, Joshua S - DOT
Subject: Re: 10901600, IH 43 and USH 12 interchange load ratings
Date: Friday, March 30, 2018 9:51:45 PM
Attachments: image002.png
image003.png

Hello Kevin,

Sorry for the delayed response. I think in this circumstance the nominal increase in the load rating is not worth the effort to add shear studs.

Thanks,
Andrew

From: Wood, Kevin <kevin.wood@graef-usa.com>
Sent: Thursday, March 22, 2018 4:00 PM
To: Smith, Andrew D - DOT
Cc: Dietsche, Joshua S - DOT; Jenks, Julie - DOT
Subject: RE: 10901600, IH 43 and USH 12 interchange load ratings

Andrew,

Just following up to see if you had any further thoughts for the items highlighted in yellow below. We'll be issuing an amendment for this project and whether to strengthen/not to strengthen will affect the hours.

I will likely be out in the field all day tomorrow but will be available talk if needed. Thanks for your help.

Kevin Wood, P.E., S.E.
Principal

GRAEF

From: Wood, Kevin
Sent: Wednesday, February 28, 2018 1:57 PM
To: 'Smith, Andrew D - DOT' <Andrew.Smith@dot.wi.gov>; 'Jenks, Julie - DOT'

<Julie.Jenks@dot.wi.gov>
Cc: Dietsche, Joshua S - DOT <Joshua.Dietsche@dot.wi.gov>
Subject: RE: 10901600, IH 43 and USH 12 interchange load ratings

Julie and Andrew,

I wanted to follow up with a summary of the conversation Andrew and I just had regarding a few bridges for our IH-43 project in Walworth Co.

Our first topic was in regards to the potential raising of bridges B-64-63 and B-64-64 (IH-43 over USH 12). Andrew was not involved with decisions to either "raise to extent possible with the existing girders", or "not to raise". Julie, would you therefore look back in your correspondence to see who at BOS I should talk to about the final decision. We'll need to make certain all are aware of the DAAR report findings, notably the resulting pile overloads when the bridge was raised and current LRFD loading was used.

Second, Andrew and I discussed load ratings for B-64-63/B-64-64 and whether or not new shear studs should be detailed over the -M regions. As noted in my February 20th e-mail below, item 3 notes an inventory rating of HS-16.8. The corresponding operating rating is HS-28.0. If shear studs are added to make the girders fully composite as noted in item 4, the inventory rating increases to HS-18.4 and the operating rating is HS-30.8. Chapter 40 of the Bridge Manual notes that shear studs generally should not be added to steel girders during a redeck (WBM 24.7.1 policy item). We will also need concurrence that although these bridges carry IH-43 over USH 12, and inventory rating less than HS-20 is acceptable (WBM 40.6 policy item).

Finally, we discussed the load rating procedure for B-64-105 where all girders are straight except for the north fascia. The north fascia is kinked by 0^43'10" at the west field splice. Because the kink produces a pseudo "inside of curve curvature" and this is an overlay project, we agreed that a line girder analysis of this fascia is appropriate.

Andrew, please follow up with any additional information or corrections.

Regards,

Kevin Wood, P.E., S.E.
Principal

GRAEF

One Honey Creek Corporate Center
125 South 84th Street, Suite 401
Milwaukee, Wisconsin 53214-1470

414 / 259 1500 office
414 / 266 9144 direct

From: Smith, Andrew D - DOT [mailto:Andrew.Smith@dot.wi.gov]
Sent: Friday, February 23, 2018 11:57 AM
To: Wood, Kevin <kevin.wood@graef-usa.com>
Cc: Dietsche, Joshua S - DOT <Joshua.Dietsche@dot.wi.gov>
Subject: RE: 10901600, IH 43 and USH 12 interchange load ratings

Thanks for the info Kevin. Looks like you tried to get everything you could out of it.

I don't need anything else. I forwarded to Josh incase he had any follow up questions.

Thanks,
Andrew

From: Wood, Kevin [mailto:kevin.wood@graef-usa.com]
Sent: Tuesday, February 20, 2018 2:49 PM
To: Jenks, Julie - DOT <Julie.Jenks@dot.wi.gov>; Smith, Andrew D - DOT <Andrew.Smith@dot.wi.gov>
Subject: RE: 10901600, IH 43 and USH 12 interchange load ratings

Julie and Andrew,

I checked the load rating models for both the redeck and overlay conditions. Limiting the strength to the plastic moment had already been applied, but allowing moment redistribution at the piers was not. After changing the moment redistribution setting, the ratings did not change because the bottom flanges over the pier are not compact. Ratings, however, are being controlled by +M in span 4 where the girder is compact. Andrew, let me know if you need any more details.

As a reminder from a few months ago, here are the load rating results:

1. Concrete overlay with the existing parapet configuration – **HS-14.6 Inventory**. The model includes an 8.5" thick deck effective for composite action in the +M regions. The concrete overlay has a variable thickness to bring the cross slopes up to 2%. The minimum concrete overlay thickness is 1.5".
2. Concrete overlay with a facing applied to the existing parapets to mimic a 42SS parapet shape – **HS-13.0 Inventory**. The model includes an 8.5" thick deck effective for composite action in the +M regions. The concrete overlay has a variable thickness to bring the cross slopes up to 2%. The minimum concrete overlay thickness is 1.5".
3. New deck with 42SS parapets – **HS-16.8 Inventory**. The model includes an 9" thick deck effective for composite action in the +M regions. Cross slopes are 2%.
4. New deck with 42SS parapets and fully composite action assuming shear studs will be added to the girders in the -M regions (this is not a standard rehabilitation approach per the Bridge Manual, see below)– **HS-18.4 Inventory**. The model includes an 9" thick deck effective for

composite action. Cross slopes are 2%.

Regards,

Kevin Wood, P.E., S.E.
Principal

GRAEF

One Honey Creek Corporate Center
125 South 84th Street, Suite 401
Milwaukee, Wisconsin 53214-1470

414 / 259 1500 office
414 / 266 9144 direct

From: Jenks, Julie - DOT [mailto:Julie.Jenks@dot.wi.gov]
Sent: Tuesday, January 23, 2018 7:50 AM
To: Wood, Kevin <kevin.wood@graef-usa.com>
Cc: Granados, Victor <Victor.Granados@graef-usa.com>
Subject: RE: 10901600, IH 43 and USH 12 interchange load ratings

Have the ratings been revised per Andrews recommendations below?

Julie Jenks
Project Manager - PDS Unit 5
WisDOT - SE Region
141 NW Barstow Street
PO Box 798
Waukesha, WI 53187-0798
262-548-6462

From: Wood, Kevin [mailto:kevin.wood@graef-usa.com]
Sent: Wednesday, December 20, 2017 10:11 AM
To: Smith, Andrew D - DOT <Andrew.Smith@dot.wi.gov>
Cc: Dietsche, Joshua S - DOT <Joshua.Dietsche@dot.wi.gov>; Granados, Victor <Victor.Granados@graef-usa.com>; Jenks, Julie - DOT <Julie.Jenks@dot.wi.gov>; Kiepczynski, Ashley - DOT <Ashley.Kiepczynski@dot.wi.gov>
Subject: RE: 10901600, IH 43 and USH 12 interchange load ratings

Andrew,

Thanks for your suggestions. You are correct in that the load ratings we are investigating are being

used for deciding whether to redeck vs. overlay using the existing parapets or refacing the existing parapets. We'll review our MDX line girder models and make the Mp/moment redistribution changes if allowed and if not already done. If ratings do not improve, another option is to build a more complex grid model and use direct lane loading.

Regards,

Kevin Wood, P.E., S.E.
Principal

GRAEF

One Honey Creek Corporate Center
125 South 84th Street, Suite 401
Milwaukee, Wisconsin 53214-1470

414 / 259 1500 office
414 / 266 9144 direct

From: Smith, Andrew D - DOT [mailto:Andrew.Smith@dot.wi.gov]
Sent: Wednesday, December 20, 2017 9:41 AM
To: Wood, Kevin <kevin.wood@graef-usa.com>
Cc: Dietsche, Joshua S - DOT <Joshua.Dietsche@dot.wi.gov>
Subject: 10901600, IH 43 and USH 12 interchange load ratings

Hello Kevin,

Josh asked me to reach out to you about the subject line project… structures B-64-63/64. It seems that the ratings are hovering around HS14-18 for inventory, depending on the direction to take for rehab?

I saw that MDX was mentioned, so I just wanted to see if you had investigated the following to see if it might help…
Are these compact girders, can Mp be used?
Try the condition "Allow Moment Redistribution"… we don't allow this per BM policy, but that is more from a design standpoint, not a rating.

Let me know your thoughts, thanks

Andrew

Scope Change Documentation

From: Landini, Anthony P - DOT
To: Jenks, Julie - DOT
Cc: Ksontini, Najoua - DOT; Wood, Kevin
Subject: RE: I-43 Walworth County 1090-16-00 - change of scope and structure survey report requirements
Date: Tuesday, March 13, 2018 4:15:34 PM
Attachments: image001.png

Julie

Start with Region Maintenance.

I think it eventually ends up in Central Office Bureau of Highway Maintenance with this gentleman:

Adams, Michael - DOT (DTSD Consultant) <Michael.Adams@dot.wi.gov>

tony

From: Jenks, Julie - DOT
Sent: Tuesday, March 13, 2018 2:57 PM
To: Landini, Anthony P - DOT <Anthony.Landini@dot.wi.gov>
Cc: Ksontini, Najoua - DOT <najoua.ksontini@dot.wi.gov>; Wood, Kevin <kevin.wood@graef-usa.com>
Subject: RE: I-43 Walworth County 1090-16-00 - change of scope and structure survey report requirements

Tony,

Under the Redeck changes you state, "B-64-107 and 126 have bridge deck weather sensors for determining when to apply de-icing chemicals. Coordinate the replacement of these systems.". Who do we coordinate with BOS or region maintenance? Do we need to confirm that the systems should be replaced? Thanks

Julie Jenks
Project Manager - PDS Unit 5
WisDOT - SE Region
141 NW Barstow Street
PO Box 798
Waukesha, WI 53187-0798
262-548-6462

From: Landini, Anthony P - DOT
Sent: Thursday, March 08, 2018 11:32 AM
To: Wood, Kevin <kevin.wood@graef-usa.com>
Cc: Ksontini, Najoua - DOT <najoua.ksontini@dot.wi.gov>; Jenks, Julie - DOT <Julie.Jenks@dot.wi.gov>
Subject: RE: I-43 Walworth County 1090-16-00 - change of scope and structure survey report requirements

Kevin

For change from concrete overlay to REDECK:

Resubmit SSR form, preliminary plans, and deck investigation/evaluation report.

Include a statement in SSR that the proposed work meets the following BM 40.4 requirement for "major rehab work, build to current standards such as safety parapets, full shoulder widths, etc.". If the proposed bridge clear width does not meet current standards indicate that it will be justified and approved in the DSR.

Include in preliminary plans staging cross sections if bridge will not be closed for re-decking.

For B-64-63 require bearings be replaced before new deck is poured. Note that it appears from the photographs the existing bearings were not placed in accordance with original plans. Adjust detailing of new bearings as required.

B-64-107 and 126 have bridge deck weather sensors for determining when to apply de-icing chemicals. Coordinate the replacement of these systems.

Structural approach slabs will not be required because it is unlikely the existing abutments can meet the demand of the increased loading.

For add thin POLYMER OVERLAY

I'm sure you are aware that there is a minimum waiting period of 28 days and moisture contents maximum limits before the thin polymer overlay can be applied. Account for this in the construction schedule and staging requirements. If the structures our opened to traffic before the overlay is applied it is likely shot blasting and cleaning will be required. We will use this email as documentation and no further submittal is required.

For add SCOUR COUNTERMEASURES (RIP RAP)

Send us email with description of proposed work supplemented with annotated photographs and/or plan view showing locations. Include information indicating how the proposed work will affect hydraulic performance.

For add EPOXY CRACK INJECTION FOR GIRDERS

We will use this email as documentation and no further submittal is required.

For the two new bridges, normal preliminary plan submittal.

I assume Graef is lead on this project and will transmit this information to others as required.

Tony

From: Wood, Kevin [mailto:kevin.wood@graef-usa.com]
Sent: Tuesday, March 06, 2018 10:19 AM
To: Landini, Anthony P - DOT <Anthony.Landini@dot.wi.gov>
Cc: Ksontini, Najoua - DOT <najoua.ksontini@dot.wi.gov>; Jenks, Julie - DOT <Julie.Jenks@dot.wi.gov>
Subject: RE: I-43 Walworth County 1090-16-00 - change of scope and structure survey report requirements

Tony,

Please see below. Color coding is change from concrete overlay to REDECK, add thin POLYMER OVERLAY for increased friction, add SCOUR COUNTERMEASURES (RIP RAP), and add EPOXY CRACK INJECTION FOR GIRDERS.

There are also two new bridges added to our scope that will add thin POLYMER OVERLAY for increased friction to B-64-122 and B-64-123. These two bridges will require SSRs.

Thanks for your help Tony,

Kevin Wood, P.E., S.E.
Principal

GRAEF

One Honey Creek Corporate Center
125 South 84th Street, Suite 401
Milwaukee, Wisconsin 53214-1470

414 / 259 1500 office
414 / 266 9144 direct

From: Landini, Anthony P - DOT [mailto:Anthony.Landini@dot.wi.gov]
Sent: Tuesday, March 06, 2018 10:05 AM
To: Wood, Kevin <kevin.wood@graef-usa.com>
Cc: Ksontini, Najoua - DOT <najoua.ksontini@dot.wi.gov>
Subject: RE: I-43 Walworth County 1090-16-00 - change of scope and structure survey report requirements

Kevin

Which of the following 1090-16-00 project structures do these changes apply too? You can just highlight structure number. I take a quick look at the reply tomorrow.

Tony

Bridge #	Mega Project	Proposed Work	Design ID	Construction ID	Feature On/Feature Over	Consultant
B-64-114	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over CTH F	CORRE
B-64-115	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over CTH F	CORRE
B-64-128	STATE	Painting	1090-16-00	1090-16-70	Lawson School Rd over IH 43	CORRE
B-64-139	STATE	Surface Repair	1090-16-00	1090-16-70	IH 43 over Swawn Creek	CORRE
B-64-140	STATE	Surface Repair	1090-16-00	1090-16-70	IH 43 over Branch Jackson Creek	CORRE
B-64-141	STATE	Surface Repair	1090-16-00	1090-16-70	IH 43 over Branch Jackson Creek	CORRE
B-64-142	STATE	Surface Repair	1090-16-00	1090-16-70	IH 43 over Branch Little Turtle Cr	CORRE
B-64-144	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over Westbound Lane Rd	CORRE
B-64-145	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over Westbound Lane Rd	CORRE
B-64-63	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over USH 12	GRAEF
B-64-64	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over USH 12	GRAEF
B-64-105	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over CTH H	GRAEF
B-64-106	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over CTH H	GRAEF
B-64-107	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over Centralia Rd & WSOR RR	GRAEF
B-64-108	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over Centralia Rd & WSOR RR	GRAEF
B-64-109	STATE	Concrete Overlay	1090-16-00	1090-16-70	STH 67 over IH 43	GRAEF
B-64-116	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over Mound Rd	GRAEF
B-64-117	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over Mound Rd	GRAEF
B-64-124	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over WSOR RR	GRAEF
B-64-125	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over WSOR RR	GRAEF
B-64-135	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over WSOR RR	GRAEF
B-64-136	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over WSOR RR	GRAEF
B-64-137	STATE	Surface Repair	1090-16-00	1090-16-70	IH 43 SB over North Rd & Turtle Cr	GRAEF
B-64-138	STATE	Surface Repair	1090-16-00	1090-16-70	IH 43 NB over North Rd & Turtle Cr	GRAEF
B-64-118	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over STH 50	MICHAEL BAKER CORP
B-64-119	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over STH 50	MICHAEL BAKER CORP
B-64-120	STATE	Abutment Repair	1090-16-00	1090-16-70	Borg Road over IH 43	MICHAEL BAKER CORP
B-64-126	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over CTH X (Beloit Rd)	MICHAEL BAKER CORP
B-64-127	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over CTH X (Beloit Rd)	MICHAEL BAKER CORP
B-64-132	STATE	Painting	1090-16-00	1090-16-70	Wisconsin Street over IH 43	MICHAEL BAKER CORP
B-64-133	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 SB over USH 14	MICHAEL BAKER CORP
B-64-134	STATE	Concrete Overlay	1090-16-00	1090-16-70	IH 43 NB over USH 14	MICHAEL BAKER CORP

From: Ksontini, Najoua - DOT
Sent: Monday, March 05, 2018 3:11 PM
To: Landini, Anthony P - DOT <Anthony.Landini@dot.wi.gov>
Subject: FW: I-43 Walworth County 1090-16-00 - change of scope and structure survey report requirements

Tony,

FYI, and can you respond to Kevin's questions below.

Thanks
Najoua

From: Wood, Kevin [mailto:kevin.wood@graef-usa.com]
Sent: Monday, March 05, 2018 2:26 PM
To: Ksontini, Najoua - DOT <najoua.ksontini@dot.wi.gov>
Cc: Sadowski, Jason <Jason.Sadowski@mbakerintl.com>; Bill Hove <bhove@correinc.com>; Jenks, Julie - DOT <Julie.Jenks@dot.wi.gov>
Subject: I-43 Walworth County 1090-16-00 - change of scope and structure survey report requirements

Najoua,

Thanks for speaking this afternoon. So that everyone is on the same page, here is what we discussed:

A. For the 4 bridges being changed from concrete overlays to deck replacements, revised structure survey reports will be required.
B. For bridges remaining as concrete overlays, but now having a thin polymer overlay added, it is not necessary to resubmit the structure survey reports. We will simply e-mail you with the affected bridge numbers and you will make sure the final plan reviewers are aware of the scope change.

Here are a few more follow up questions:

1. For the redecks, which exhibits should be resubmitted (SSR form, preliminary plans, others?)?
2. One of our culvert rehabs, we will now have scour countermeasures (riprap) added. Will this change follow the same procedure as item B above?
3. For the redecks, please confirm that a structure approach slab will not be required as noted in the WBM 12.11 policy item.

Thanks again for your help,

Kevin Wood, P.E., S.E.
Principal

GRAEF

One Honey Creek Corporate Center
125 South 84th Street, Suite 401
Milwaukee, Wisconsin 53214-1470

414 / 259 1500 office
414 / 266 9144 direct

From:	Landini, Anthony P - DOT
To:	Wood, Kevin
Cc:	Bill Hove; Jason Sadowski (jason.sadowski@mbakerintl.com); Filtz, Dean J - DOT; Ksontini, Najoua - DOT; Walbrun, Alexander W - DOT; Revello, Steven - DOT
Subject:	RE: 1090-16-00 I-43 Walworth Co. - scope change and SSR resubmit needs
Date:	Wednesday, September 05, 2018 10:34:41 AM
Attachments:	image001.png

Kevin

As we discussed over the phone you will not need to re-submit revised SSR's for the scope changes. Instead take the latest spreadsheet and turn it into a pdf called something like SSR scope changes 090618.pdf. Make sure all the appropriate columns show up. At the top of the file include something like: Date of scope changes. Columns X is review comments by others in BOS made xx-xx-xxxx, Y is Region Bridge Maintenance reply comments, and F through W is the current scope as directed by Region. Cells highlighted in red are items that were removed from the previously approved preliminary plan scope.

Then send 3 mail messages to Najoua, Alex, and I, copying others as needed. The subject line of each message should be:
1090-16-00 Scope changes B-64-63,64,105,107,108,109,116,117,124,125,135,136,137,138,118,119,120,126,127,132,133,134
1090-16-00 Scope changes B-64-122,123
1090-16-00 Scope changes B-64-114,115,128,139,140,141,142,144,145

Attached the pdf file to each message. Indicate you are sending this as lead Consultant on the project.

Alex

When we get these messages, file the email in the 3 shared mail folders, and copy the pdf into the design data folder for each bridge.

Tony

From: Wood, Kevin [mailto:kevin.wood@graef-usa.com]
Sent: Sunday, September 02, 2018 10:11 AM
To: Landini, Anthony P - DOT <Anthony.Landini@dot.wi.gov>; Filtz, Dean J - DOT <Dean.Filtz@dot.wi.gov>
Cc: Bill Hove <bhove@correinc.com>; Jason Sadowski (jason.sadowski@mbakerintl.com) <jason.sadowski@mbakerintl.com>
Subject: 1090-16-00 I-43 Walworth Co. - scope change and SSR resubmit needs

Dean and Tony,

With the recent scope changes for our I-43 Walworth Co. project (eliminating several concrete overlays, joint replacements, and steel girder painting), will there be a need to resubmit the Structure Survey Reports to reflect the reduction in scope? Or, will an e-mail summarizing the changes be sufficient as had been done earlier? In case you were not in the loop Tony, attached is a spreadsheet showing the eliminated scope items in red.

Thanks,

Kevin Wood, P.E., S.E.
Principal

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