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STANDARD DETAIL DRAWINGS

08A5-16C

INLET COVERS TYPE F, HM, HM-S, S, V, HM-GJ, & HM-GJ-S

08C1-5

INLETS TYPE 1, 2, 3 & 4

08C5-2

INLETS TYPE 8, 9, 10, AND 11

08D2-4

CONCRETE SURFACE DRAIN FLUME TYPE AT STRUCTURES

08E8-3

TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

08E9-5

SILT FENCE

08E10-2

INLET PROTECTION TYPE A, B, C AND D

08F1-11

APRON ENDWALLS FOR CULVERT PIPE

08F4-5

JOINT TIES FOR CONCRETE PIPE

12A1-2

SLOPE PAVING-STRUCTURES (CONCRETE CAST-IN-PLACE)

13A5-3A

ASPHALTIC SHOULDER RUMBLE STRIP, MILLING

13A5-3B

ASPHALTIC SHOULDER RUMBLE STRIP, MILLING

13C1-11

CONCRETE PAVEMENT LONGITUDINAL JOINTS AND PAVEMENT TIES

14B7-10A

CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"

14B7-10B

CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"

14B7-10C

CONCRETE BARRIER TEMPORARY PRECAST, 12'-6" BENDING DIAGRAM

14B7-10D

CONCRETE BARRIER TEMPORARY PRECAST, 12'-6" ANCHORING DETAIL AND CRASH CUSHION

14B7-10E

CONCRETE BARRIER TEMPORARY PRECAST, 10'-0"

14B7-10F

CONCRETE BARRIER TEMPORARY PRECAST, 10'-0" AND PORTABLE CRASH CUSION

14B10-1

CLASS "A" STEEL PLATE BEAM MEDIAN GUARD

14B15-4A

STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS

14B15-4B

STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS

14B16-3A

CLASS "A" STEEL PLATE BEAM GUARD END TREATMENT WITH ANCHORAGE, TYPE 2

14B16-3B

CLASS "A" STEEL PLATE BEAM GUARD END TREATMENT WITH ANCHORAGE, TYPE 2

14B18-4A

STEEL PLATE BEAM GUARD, CLASS "A" (AT BRIDGES, OBSTACLES AND SIDEROADS/DRIVEWAYS)

14B18-4D

STEEL PLATE BEAM GUARD, CLASS "A", BULLNOSE INSTALLATION AT MEDIAN PIERS AND SIGN SUPPORTS

14B20-5A

STEEL THRIE BEAM STRUCTURE APPROACH

14B20-5B

STEEL THRIE BEAM STRUCTURE APPROACH, CONNECTION TO SQUARE END AND VERTICAL FACED PARAPETS

14B22-3A

CONCRETE BARRIER, SINGLE-FACED (WITH ANCHORAGE)

14B22-3B

CONCRETE BARRIER, SINGLE-FACED (WITH ANCHORAGE)

14B24-4A

STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL

14B24-4B

STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL

14B24-4C

STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL

15A2-5

DELINEATOR POST, DELINEATOR BRACKET AND DELINEATOR

15A3-1

MARKER POSTS, FLEXIBLE, FOR CULVERT END

15C4-1

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

15C8-9A

PAVEMENT MARKING (MAINLINE)

15C8-10E

PAVEMENT MARKING (ISLANDS, STOP LINE & CROSS WALK)

15C8-9F

PAVEMENT MARKING (RAMPS & GORES)

15C12-2

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

15C18-1

MEDIAN ISLAND MARKING

15D3-1

TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M.P.H. WITH BARRIER

15D12-2

TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M.P.H.

15D14-1

TRAFFIC CONTROL, TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY, SHORT-TERM (LESS THAN 24 HOURS)

15D15-1

TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE

15D16-1

TRAFFIC CONTROL, EXIT RAMP CLOSURE

GENERAL NOTES

- 1) THE LOCATIONS OF EXISTING OR PROPOSED UTILITIES, AS NOTED ON THE PLANS, ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

2) DIMENSIONS GIVEN FOR EXISTING FEATURES SHALL BE CONSIDERED AS APPROXIMATE AND SHALL BE MEASURED IN THE FIELD FOR MATCHING PURPOSES.

3) THE QUANTITIES FOR THE ITEMS WARRANTED HMA PAVEMENT, BASE AGGREGATE DENSE 3/4-INCH AND BASE AGGREGATE DENSE 1 1/4-INCH ARE MEASURED FOR PAYMENT BY THE TON. THE THICKNESSES SHOWN IN THE PLANS ARE APPROXIMATE, AND THE ACTUAL THICKNESSES WILL BE DEPENDENT ON DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

4) WHEN RAMPS ARE CLOSED, THOSE SIGNS WHICH ARE NOT APPLICABLE SHALL BE COVERED. TRAFFIC CONTROL SIGNS WHICH ARE APPLICABLE TO RAMP CLOSURES SHALL BE COVERED WHEN THE RAMPS ARE OPEN TO TRAFFIC, AS DIRECTED BY THE ENGINEER.

5) ALL DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE SEEDED, FERTILIZED, AND MULCHED OR EROSION-MATTED AS DIRECTED BY THE ENGINEER.

6) ALL TRENCHES RESULTING FROM CULVERT AND INLET IMPROVEMENTS AND REMOVAL, WHICH FALL BENEATH THE PAVEMENT STRUCTURE OF THE ROADWAY, SHALL BE BACKFILLED WITH "SPECIAL GRANULAR BACKFILL", AS DIRECTED BY THE ENGINEER.

7) APRON ENDWALLS AND ADJOINING SECTIONS OF CONCRETE PIPE SHALL BE TIED TOGETHER AS SHOWN ON THE STANDARD DETAIL DRAWINGS AND AS LOCATED IN THE MISCELLANEOUS QUANTITIES. JOINT TIES SHALL BE INCIDENTAL TO THE VARIOUS ITEMS.

8) CONTRACTOR SHALL INSTALL NEW DRAINAGE STRUCTURES MAINTAINING EXISTING INLET AND DISCHARGE ELEVATIONS. ANY MODIFICATIONS TO EXISTING FLOWLINE ELEVATIONS SHALL BE APPROVED BY THE ENGINEER. PIPE LENGTHS TO BE VERIFIED IN THE FIELD.

9) SEEDING SHALL CONSIST OF SEED MIXTURE NUMBER 30. FERTILIZER SHALL BE TYPE B.

10) ELEVATIONS SHOWN IN THIS PLAN ARE NAD 27.
- 11) WHEN PORTIONS OF EXISTING ASPHALTIC SURFACES ARE TO BE REMOVED TO ACCOMMODATE NEW CONSTRUCTION, THE TRANSVERSE LINE OF SUCH REMOVAL SHALL BE NEATLY DELINEATED WITH A SAW CUT JOINT THROUGH THE ASPHALTIC SURFACE.

12) WHEN TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IS REQUIRED TO BE USED, IT SHALL BE CUT THRU IT'S FULL WIDTH AND THICKNESS AT 25-FOOT INTERVALS TO CONTROL RAVELING.

13) ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

14) SEE HMA PAVEMENT LAYER THICKNESS TABLE FOR PROPOSED NUMBER OF LAYERS AND LAYER THICKNESS.

15) EROSION CONTROL ITEMS SHOWN ON THE PLAN ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS AND DIMENSIONS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

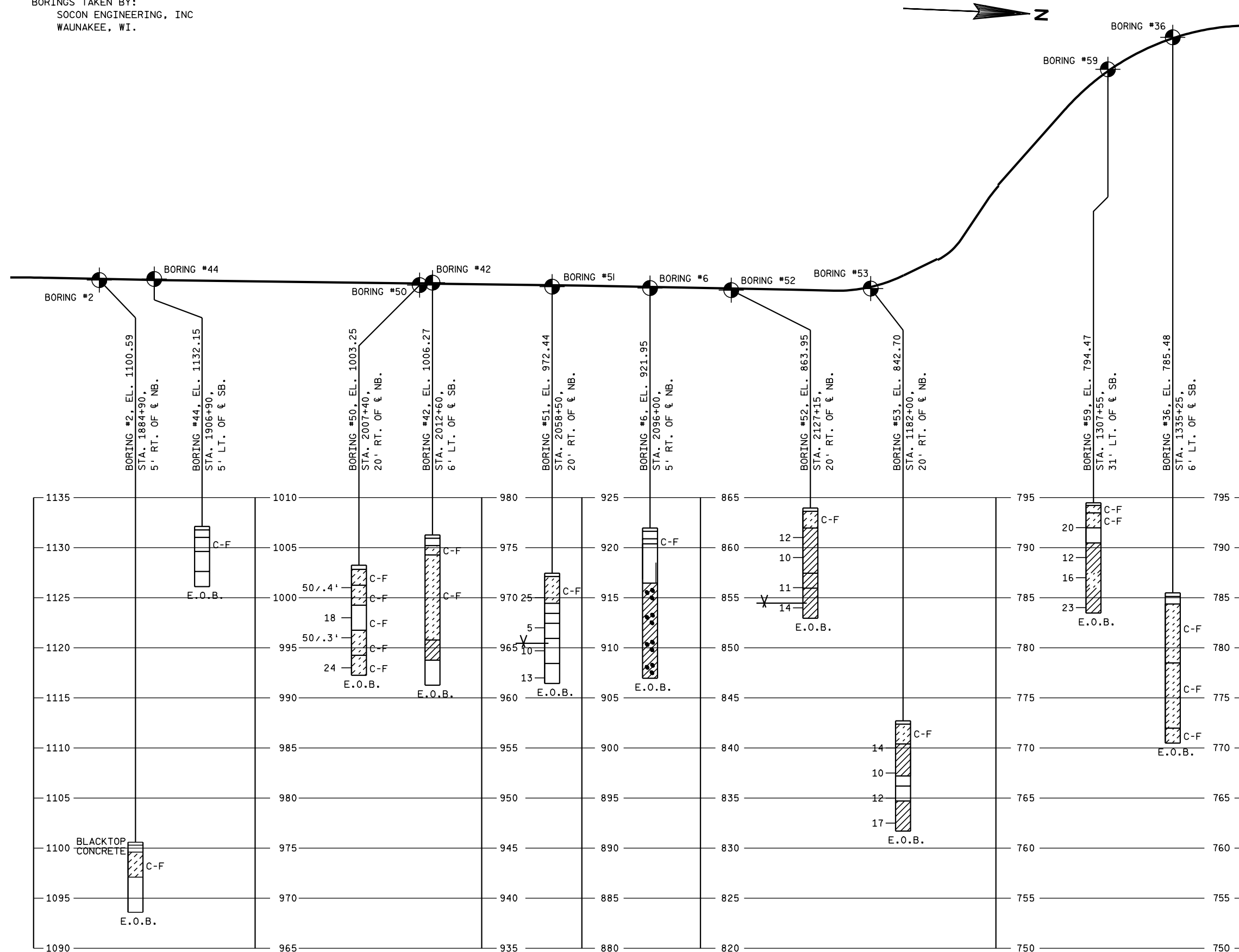
16) EXCAVATION BELOW SUBGRADE (EBS) SHALL BE MEASURED AND PAID FOR AS EXCAVATION COMMON. THE EXACT LOCATION OF EBS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

17) SHOULDER SHALL BE PAVED FULL WIDTH IN LOCATIONS WHICH REQUIRE STEEL PLATE BEAM GUARD AND SHALL BE PAVED PRIOR TO BEAM GUARD INSTALLATION.

18) REMOVE DELINEATOR POSTS THAT OBSTRUCT SHOULDER IMPROVEMENTS AND INSTALL NEW DELINEATOR POSTS AND DELINEATORS AS DIRECTED BY THE ENGINEER.

18) ANY REFERENCE TO WARRANTED SMA IN THIS PLAN SHALL BE CONSIDERED AS THE BID ITEM HMA PAVEMENT, TYPE SMA.

BORINGS TAKEN BY:  
SOCON ENGINEERING, INC  
WAUNAKEE, WI.



STATE PROJECT NUMBER

SHEET NO.

ABBREVIATIONS  
F ---- Fine M ---- Medium C ---- Coarse  
Ws ---- Weathered So ---- Sound

MATERIAL SYMBOLS

|         |      |              |
|---------|------|--------------|
| Topsoil | Silt | Sandstone    |
| Sand    | Peat | Limestone    |
| Gravel  | Clay | Igneous Rock |

## LEGEND OF PROBING

Probing No.  
Station  
Elevation

95/6 = 95 Blows for 6" Penetration  
Probing taken with a 350\* wt. Falling 18" on a 2' O.D. Point.

7 Average Blows Per Foot

Refusal 95/6

## LEGEND OF BORING

Boring No., Elev.  
Sta. & Offset

Unconfined Strength → 7.7  
Blows Per Foot Using 140\* Wt. Falling 30".  
Wash Sample  
Shelby Tube — S.T.  
Ground Water Elevation  
No Ground Water Observed Above This Elevation

Sandy Gravel  
F  
Boulders or Cobbles  
Sand  
Silty Clay  
So  
Limestone

Unless otherwise specified, the blows per foot at the locations indicated are based on driving a 2" O.D. x 1.4" I.D. split spoon sampler with a 140\* hammer having a free fall of 30". The blow count is taken in undisturbed soil immediately below a cased or open hole eliminating side friction on the drive pipe.

## SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

To obtain relative data concerning the character of material in and upon which the foundation might be built, borings and/or soundings were made at points approximately as indicated on this drawing. The data presented herein represents the findings of the subsurface explorations made. However, because the depths investigated are limited and the area of the borings and/or soundings is very small in relation to the entire area, the DEPT. OF TRANSPORTATION does not warrant conditions below the depths investigated or that the classification of material encountered in these investigations is necessarily typical of the entire site.

|   |      |               |               |
|---|------|---------------|---------------|
| No.   | Date | Revision      | By            |
| PLANS PREPARED BY<br><b>AYRES ASSOCIATES</b><br>Engineers/Architects<br>Planners/Surveyors<br>Owen Ayres & Associates Inc.<br>Waukesha, Wisconsin |      |               |               |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION  |      |               |               |
| Const. Spec.  | 1981 | Drawn By E.P. | Plans Checked |
| SUBSURFACE EXPLORATION  |      |               | SHEET OF<br>X |

PROJECT NO: 1104-12-70

HWY: USH 41

COUNTY: FOND DU LAC

SUBSURFACE EXPLORATION

SHEET

E

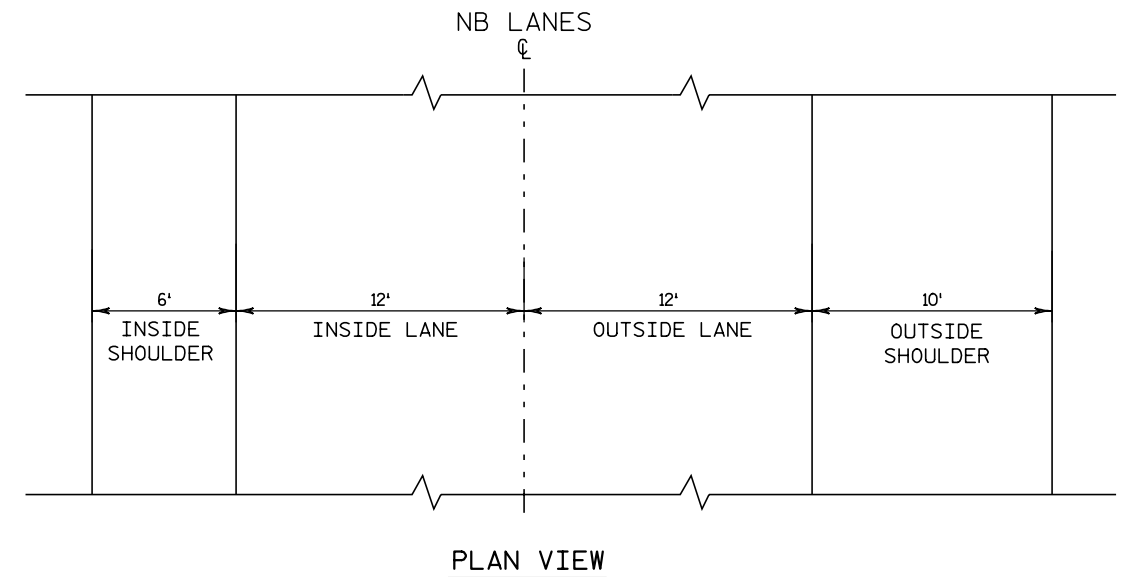
| SURFACE BORING LOG & ESTIMATED EXISTING ASPHALTIC CONCRETE<br>PAVEMENT (ACP) @ AND EDGE OF LANE THICKNESS |         |           |             |                               |                                  |   |
|---|---------|-----------|-------------|-------------------------------|----------------------------------|---|
| CORE NO.  | STATION | DIRECTION | @<br>OFFSET | CORE<br>THICKNESS<br>(INCHES) | ESTIMATED<br>ACP @ @<br>(INCHES) | ESTIMATED<br>ACP @ EDGE OF LANE<br>(INCHES) |
| C-1   | 1832+00 | NB        | 5' RT.      | 4.000                         | 4.625                            | 3.125                                       |
| B-2   | 1884+90 | NB        | 5' RT.      | 3.500                         | 4.125                            | 2.625                                       |
| C-3   | 1937+80 | NB        | 5' RT.      | 4.250                         | 4.875                            | 3.375                                       |
| C-4   | 1990+30 | NB        | 5' RT.      | 4.125                         | 4.750                            | 3.250                                       |
| C-5   | 2043+20 | NB        | 5' RT.      | 4.125                         | 4.750                            | 3.250                                       |
| B-6   | 2096+00 | NB        | 5' RT.      | 4.000                         | 4.625                            | 3.125                                       |
| P-7   | 2148+70 | NB        | 5' RT.      | 4.500                         | 5.125                            | 3.625                                       |
| C-8   | 2148+70 | NB        | 5' LT.      | 3.875                         | 4.500                            | 3.000                                       |
| B-21  | 1839+15 | NB        | 5' RT.      | 3.750                         | 4.375                            | 2.875                                       |
| C-22  | 1892+30 | NB        | 5' LT.      | 3.875                         | 4.500                            | 3.000                                       |
| P-23  | 1892+30 | NB        | 5' RT.      | 4.000                         | 4.625                            | 3.125                                       |
| C-24  | 1913+30 | SB        | 5' LT.      | 6.250                         | 6.875                            | 5.375                                       |
| C-25  | 1865+50 | SB        | 4' LT.      | 3.500                         | 4.000                            | 2.500                                       |
| P-40  | 2076+00 | SB        | 5' LT.      | 5.500                         | 6.125                            | 4.625                                       |
| C-41  | 2065+20 | SB        | 6' LT.      | 4.375                         | 5.125                            | 3.625                                       |
| B-42  | 2012+60 | SB        | 6' LT.      | 4.000                         | 4.750                            | 3.250                                       |
| C-43  | 1959+80 | SB        | 6' RT.      | 3.750                         | 4.500                            | 3.000                                       |
| B-44  | 1906+90 | SB        | 5' RT.      | 4.250                         | 4.875                            | 3.375                                       |
| C-45  | 1854+00 | SB        | 6' LT.      | 4.000                         | 4.750                            | 3.250                                       |
| C-46  |         | SB        | 6' LT.      | 3.875                         | 4.625                            | 3.125                                       |
| AVERAGE   |         |           |             | 4.250                         | 4.750                            | 3.500                                       |

ESTIMATED ACP @ @ = CORE ACP THICKNESS + (@ OFFSET \* DIFFERENCE IN CROSS-SLOPE)

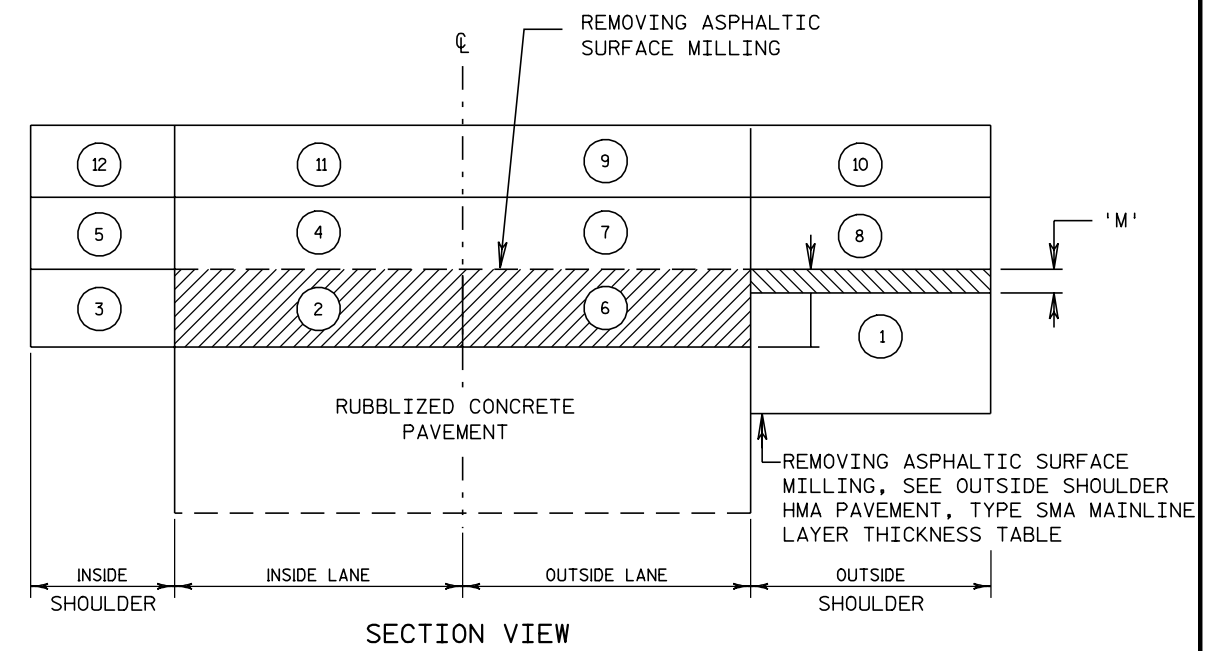
ESTIMATED ACP @ = CORE ACP THICKNESS - [(12' - @ OFFSET) \* DIFFERENCE IN CROSS-SLOPE]

NOTE: 9" CONCRETE PAVEMENT CROSS-SLOPE IS ESTIMATED AT 1% AND  
EXISTING ASPHALTIC PAVEMENT CROSS-SLOPE IS ESTIMATED AT 2%  
RESULTING IN A DIFFERENCE OF CROSS-SLOPE EQUAL TO 1%.

\* FIELD DATA COLLECTED FROM MAINLINE PAVEMENT CORES INDICATE AN EXISTING OUTSIDE LANE EDGE THICKNESS 'T' VARYING FROM 2½" TO 5⅜". CONTRACTOR SHALL FIELD VERIFY OUTSIDE LANE EDGE THICKNESS 'T' DURING STAGE 1 STAGE 3 CONSTRUCTION. TO DETERMINE AREAS OF REMOVING ASPHALTIC SURFACE MILLING REQUIRED ON OUTSIDE SHOULDER DURING STAGE 3 CONSTRUCTION. REMOVING ASPHALTIC SURFACE MILLING ON OUTSIDE SHOULDER TO BE APPROVED BY THE ENGINEER.



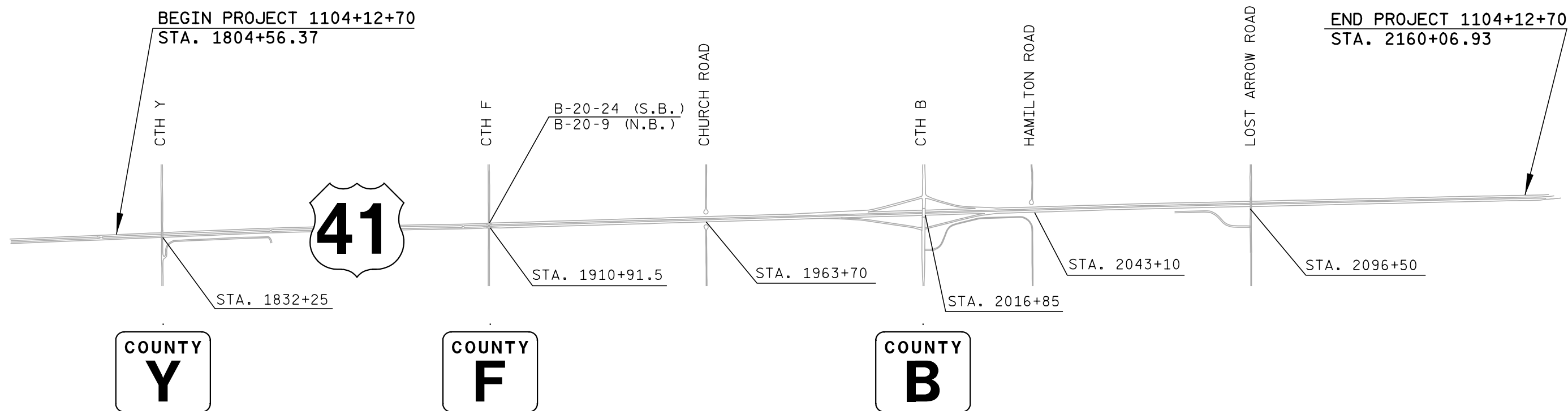
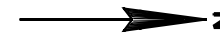
| OUTSIDE SHOULDER HMA PAVEMENT, TYPE SMA MAINLINE LAYER THICKNESS TABLE |  |  |   |   |
|--|--|--|---|---|
| EXISTING<br>OUTSIDE LANE *<br>EDGE THICKNESS 'T'                       | LOWER LAYER<br>THICKNESS<br>1" AGGREGATE (1) | LOWER LAYER<br>THICKNESS<br>¾" AGGREGATE (8) | UPPER LAYER<br>THICKNESS<br>½" AGGREGATE (10) | REMOVING ASPHALTIC<br>SURFACE MILLING<br>REQ'D. |
| < 1¾"  | 5"   | 3"   | 2¼"   | -   |
| 1½" TO 2"  | 5"   | 3"   | VARIABLE<br>2¼" TO 1¾"                        | -   |
| 2" TO 3¼"  | 5"   | VARIABLE<br>3" TO 1¾"                        | 1¾"   | -   |
| > 3¼"  | 5"   | LAYER NOT<br>REQUIRED                        | 1¾"   | MILLING THICKNESS<br>'M' = 'T' - 3¼"            |

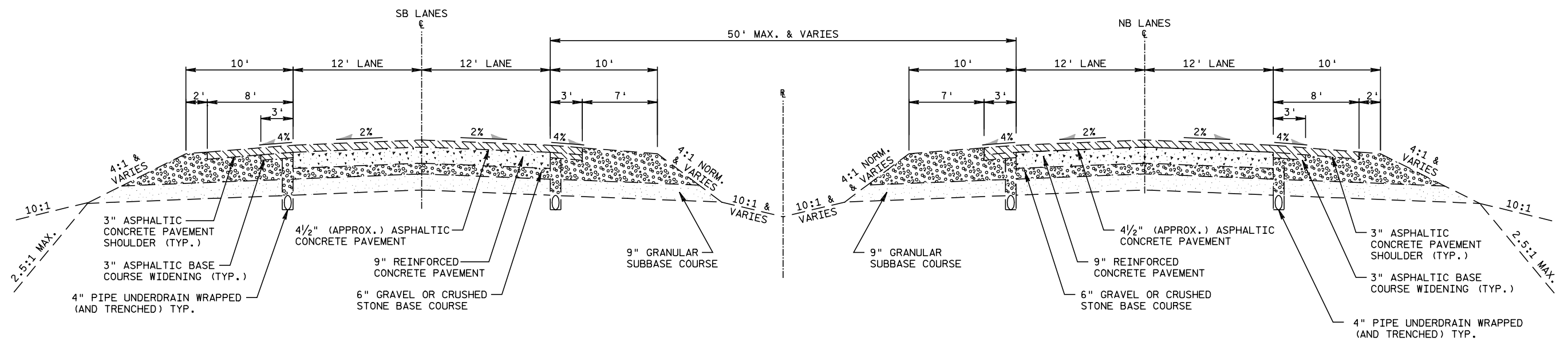


- LEGEND

|   |   |
|---|---|
| <p>① OUTSIDE SHOULDER STAGE 1</p> <p>② INSIDE LANE STAGE 2</p> <p>③ INSIDE SHOULDER STAGE 2</p> <p>④ INSIDE LANE STAGE 2</p> <p>⑤ INSIDE SHOULDER STAGE 2</p> <p>⑥ OUTSIDE LANE STAGE 3</p> | <p>⑦ OUTSIDE SHOULDER STAGE 3</p> <p>⑧ OUTSIDE SHOULDER STAGE 3</p> <p>⑨ OUTSIDE LANE STAGE 4</p> <p>⑩ OUTSIDE SHOULDER STAGE 4</p> <p>⑪ INSIDE LANE STAGE 4</p> <p>⑫ INSIDE SHOULDER STAGE 4</p> |
|---|---|

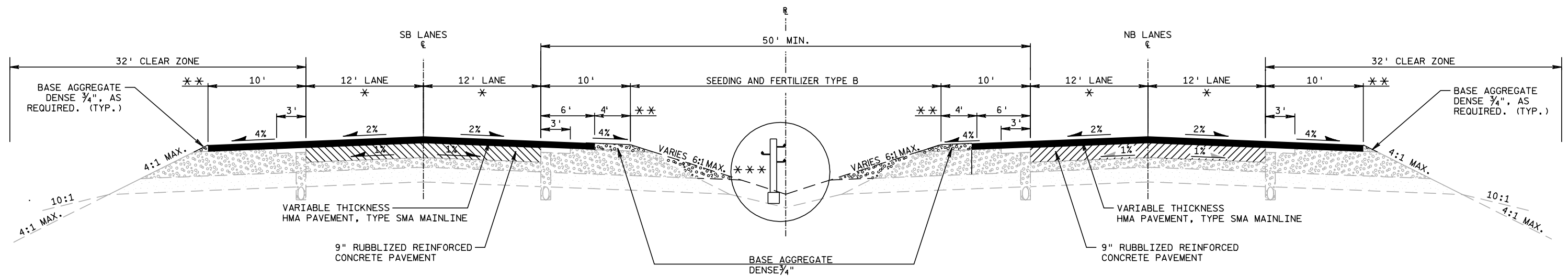
USH 41  
MILWAUKEE TO GREEN BAY ROAD  
SOUTH FOND DU LAC COUNTY LINE TO STH 175





**TYPICAL EXISTING SECTION  
MAINLINE**

STA. 1804+56.37 - STA. 2160+06.93



**TYPICAL FINISHED SECTION  
MAINLINE**

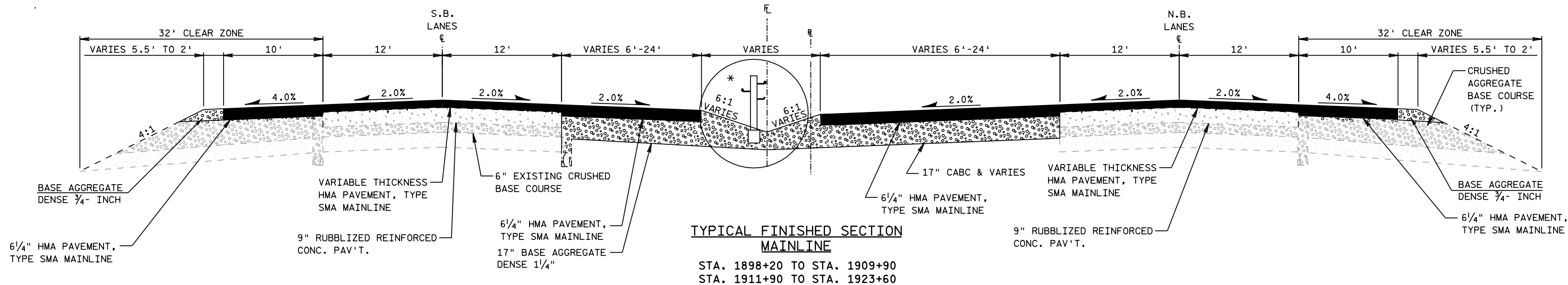
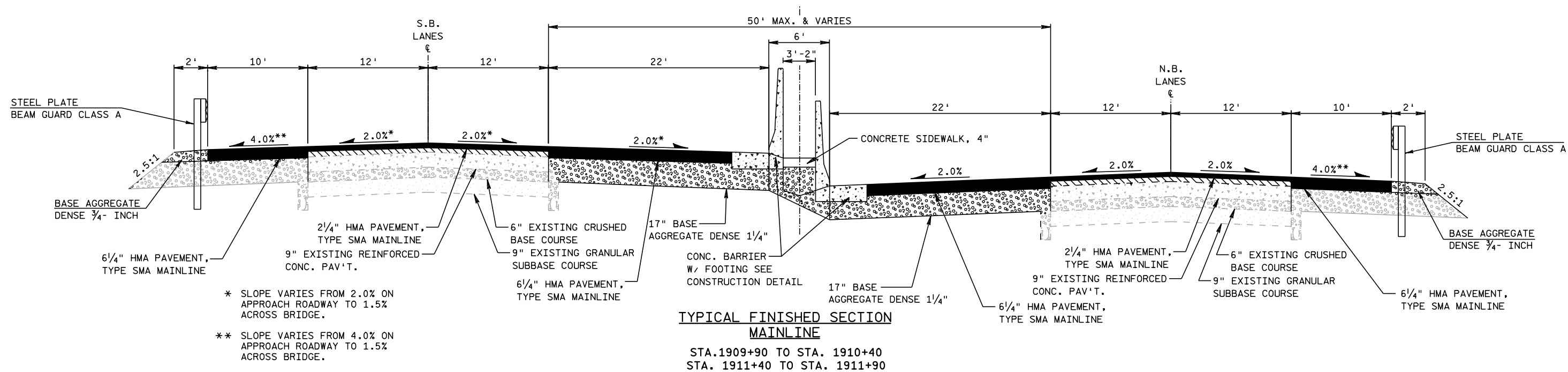
STA. 1804+56.37 - STA. 1898+20  
STA. 1923+60 - STA. 2160+06.93

\* MILL EXISTING ASPHALT PAVEMENT FULL DEPTH.  
RUBBLIZE 9" EXISTING REINFORCED CONCRETE PAVEMENT.  
OVERLAY WITH HMA PAVEMENT, TYPE SMA  
8 1/4" @ CENTERLINE 6 3/4" @ EDGE OF PAVEMENT

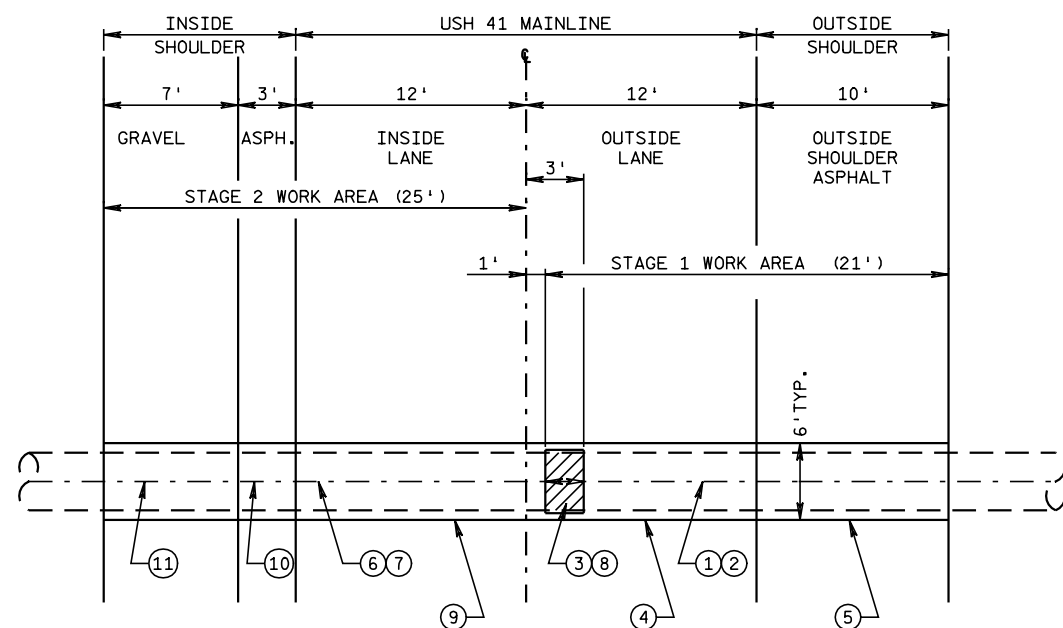
\* \* MILL EXISTING ASPHALT SHOULDERS,  
PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS.  
REPLACE WITH HMA PAVEMENT, TYPE SMA  
OUTSIDE SHOULDER SEE TABLE "SERVICE BORING LOG & ESTIMATED ACP @ AND EDGE OF LANE THICKNESS."  
INSIDE SHOULDER 6 3/4" PLACE CRUSHED AGGREGATE BASE COURSE SHOULDER, AS REQUIRED.

\* \* \* HIGH TENSION CABLE BARRIER REQD. SEE MISC. QUANTITIES  
AND CONSTRUCTION DETAILS FOR LOCATIONS.

NOTE: EXISTING CONCRETE IS AT 1% CROSS SLOPE



\* HIGH TENSION CABLE BARRIER REQD SEE MISC.  
QUANTITIES AND CONSTRUCTION DETAILS FOR LOCATIONS



PLAN VIEW SB LANES  
TYPICAL

#### NOTES:

SHOWN FOR SB LANE PIPEWORK. NB LANES SIMILAR.

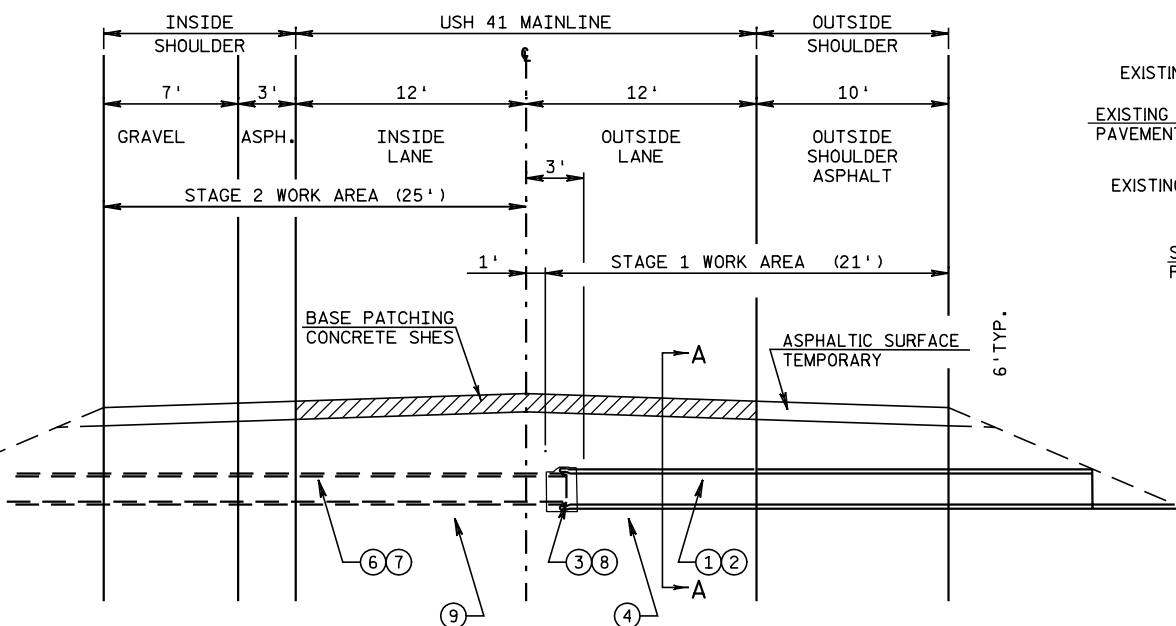
WIDTHS AND SURFACE TYPE FOR SHOULDER SHOWN  
DEPICT FINISHED CONDITION.

#### STAGE 1

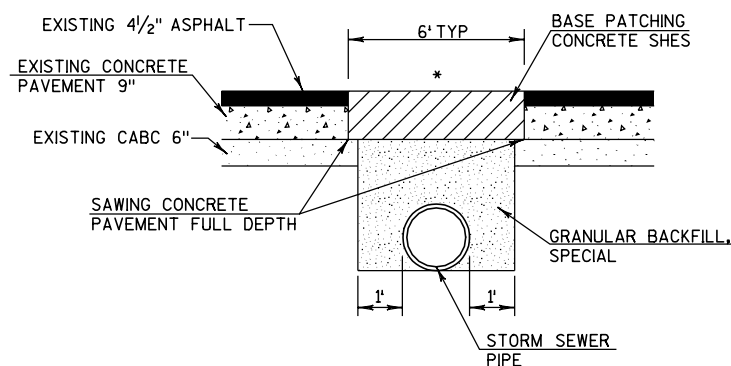
- ① REMOVE EXISTING CULVERT PIPE.
- ② CONSTRUCT NEW RCCP.
- ③ PLACE NEW RCCP BELL OVER EXISTING CULVERT PIPE,  
WRAP TEMPORARY JOINT WITH FABRIC, INCIDENTAL TO CONSTRUCTION.
- ④ SPECIAL HIGH EARLY BASE PATCHING CONCRETE REQUIRED.
- ⑤ PLACE ASPHALTIC SURFACE TEMPORARY IF PIPE  
WORK IS COMPLETED AND OUTSIDE LANE IS OPEN TO  
TRAFFIC PRIOR TO STAGE 1 MILLING.

#### STAGE 2

- ⑥ REMOVE EXISTING CULVERT PIPE.
- ⑦ CONSTRUCT NEW RCCP.
- ⑧ CONNECT NEW RCCP TO RCCP CONSTRUCTED  
IN STAGE 1.
- ⑨ SPECIAL HIGH EARLY BASE PATCHING CONCRETE REQUIRED.
- ⑪ BASE AGGREGATE DENSE  $\frac{3}{4}$ " REQUIRED.



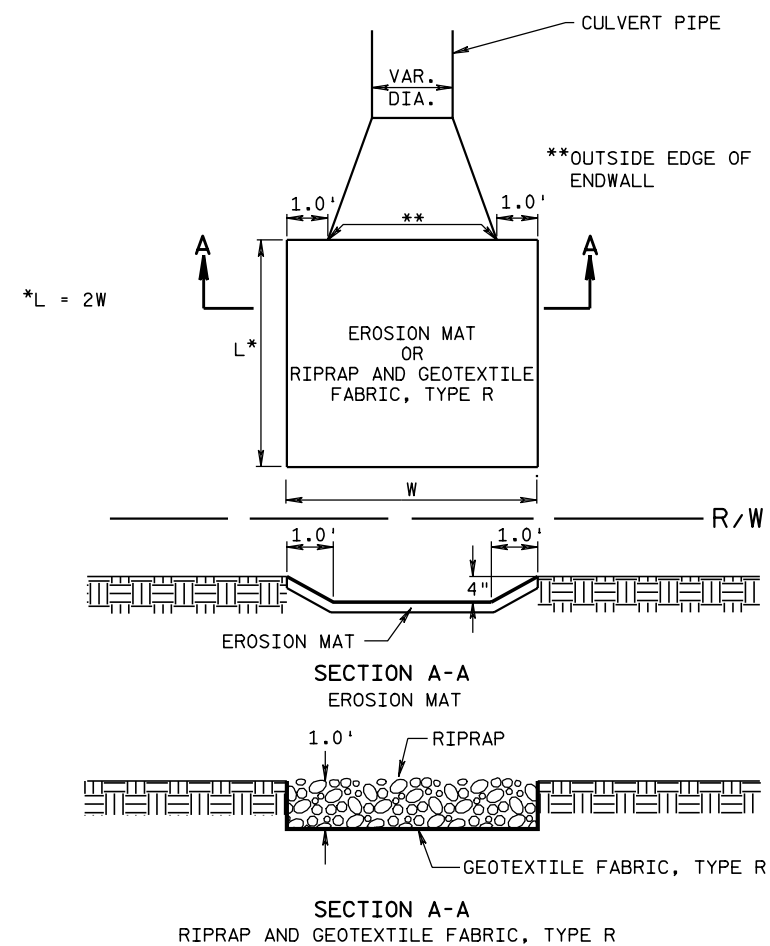
X-SECTION SB LANES



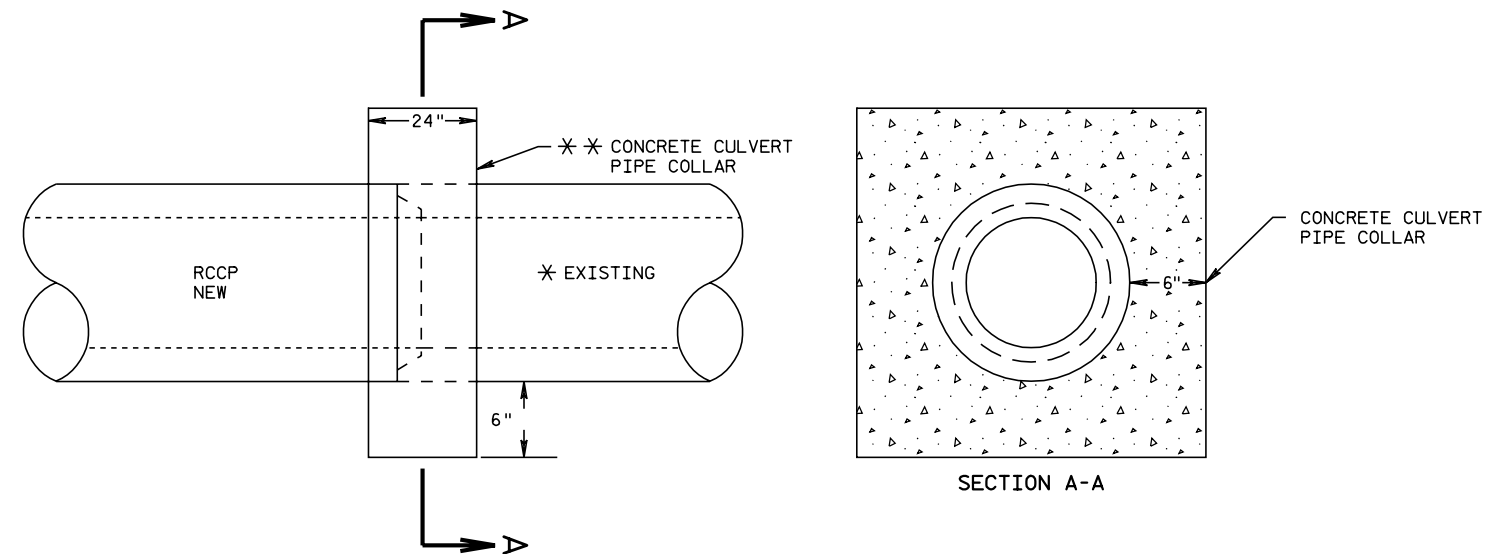
SECTION A-A

\* MILLING OF BASE PATCHING CONCRETE SHES  
REQUIRED AND PAID AS ASPHALTIC MILLING

PIPE CULVERT CONSTRUCTION DETAILS

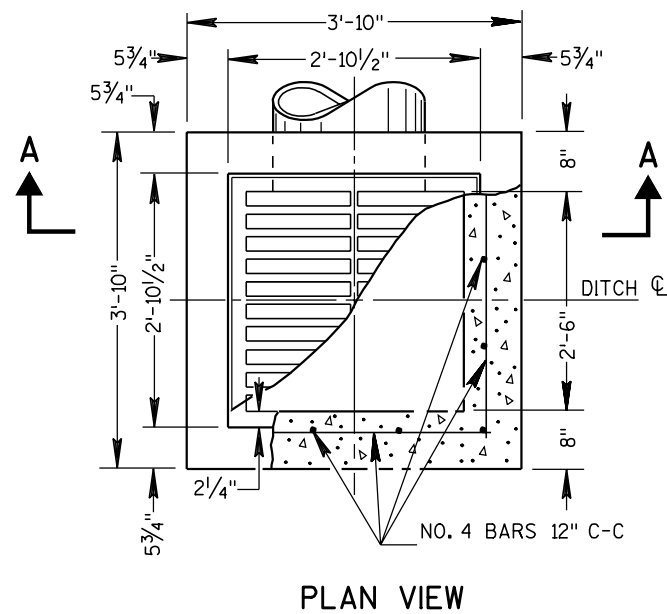


DETAIL FOR FINAL EROSION  
CONTROL AT CULVERT PIPES



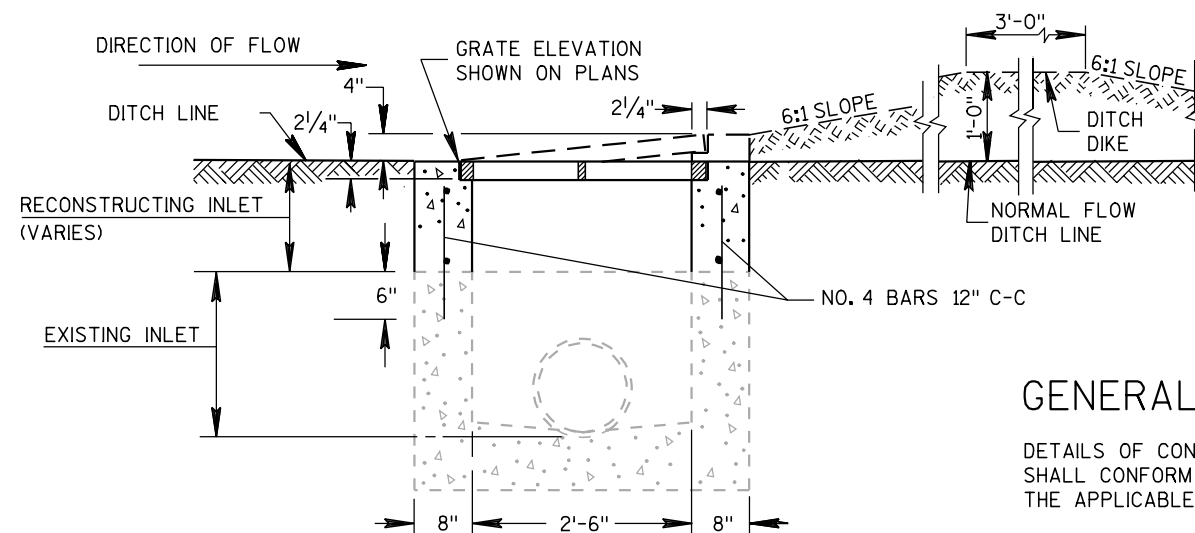
\* SHOWN AS RCCP, CPCM SIMILAR.  
\* \* SHES CONCRETE REQUIRED FOR  
CONCRETE COLLARS USED AS PERMANENT  
CONNECTION OF CULVERT PIPES UNDER  
PAVEMENT.

#### CONCRETE COLLAR DETAIL



#### RECONSTRUCTING INLET TYPE 8

(SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS)



#### GENERAL NOTES

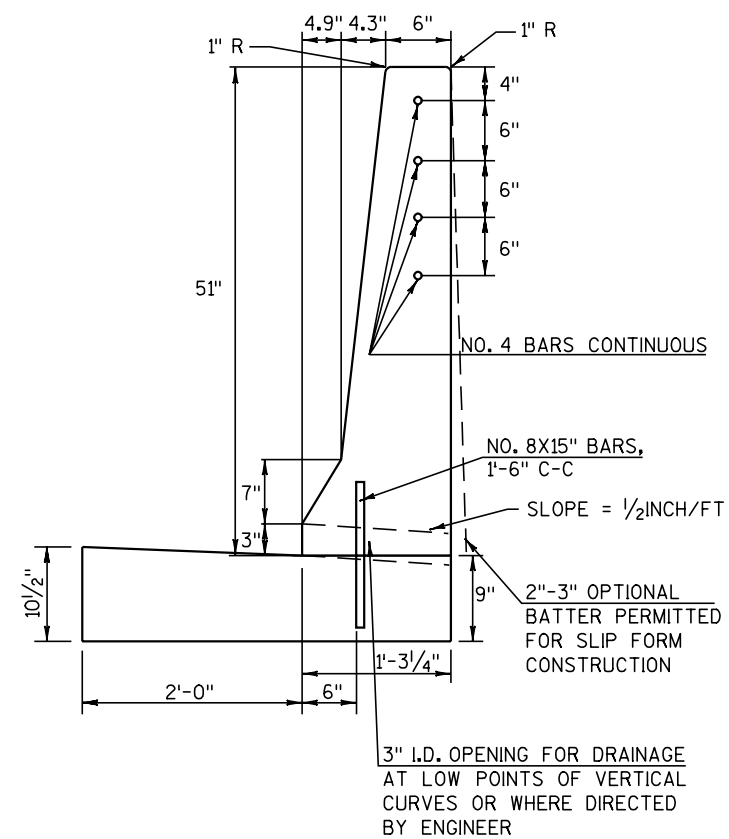
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

ALL INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, 8-MS", ETC. THIS DESIGNATION IS INTERPRETED TO MEAN THAT THE NUMBER, OR FIRST DIGIT DESIGNATES THE MASONRY PORTION OF THE STRUCTURE AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER OR IRON CASTING TO BE USED THEREWITH TO COMPRISE THE COMPLETE UNIT.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

## GENERAL NOTES

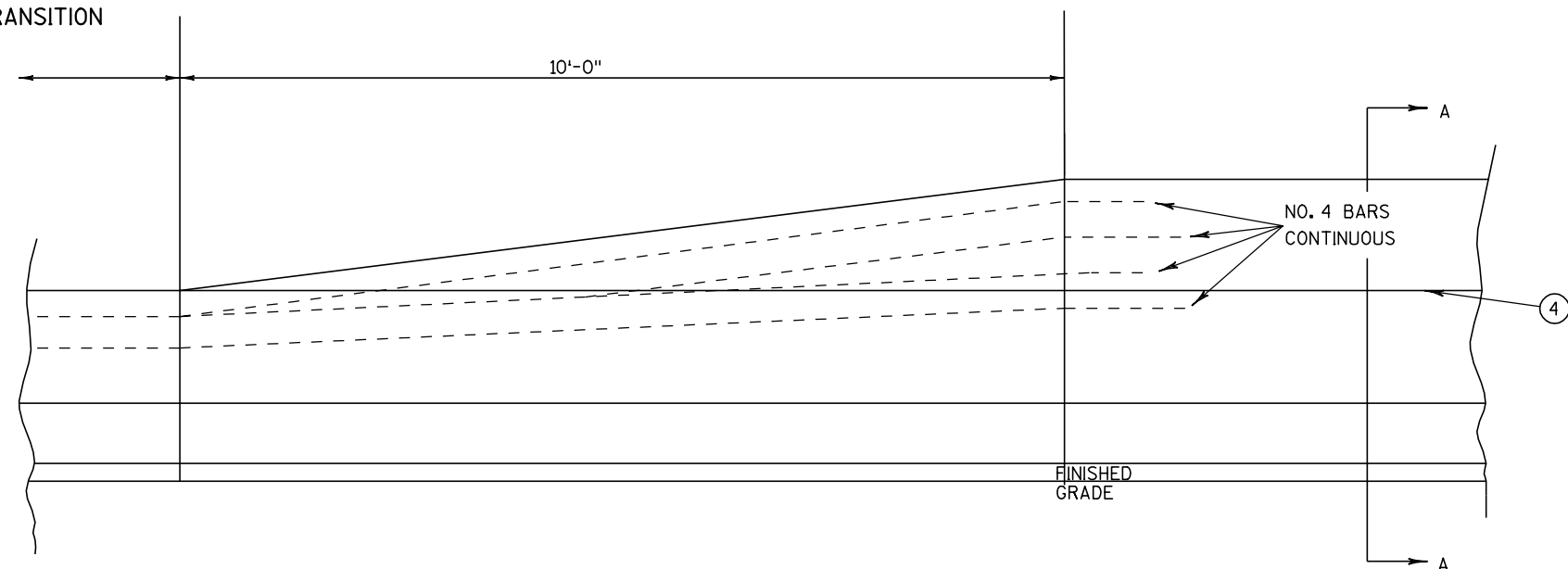
- ② 3/4 INCH EXPANSION JOINT MATERIAL  
 ④ OPTIONAL CONSTRUCTION JOINT FOR  
 SLIPPED FORM BARRIER



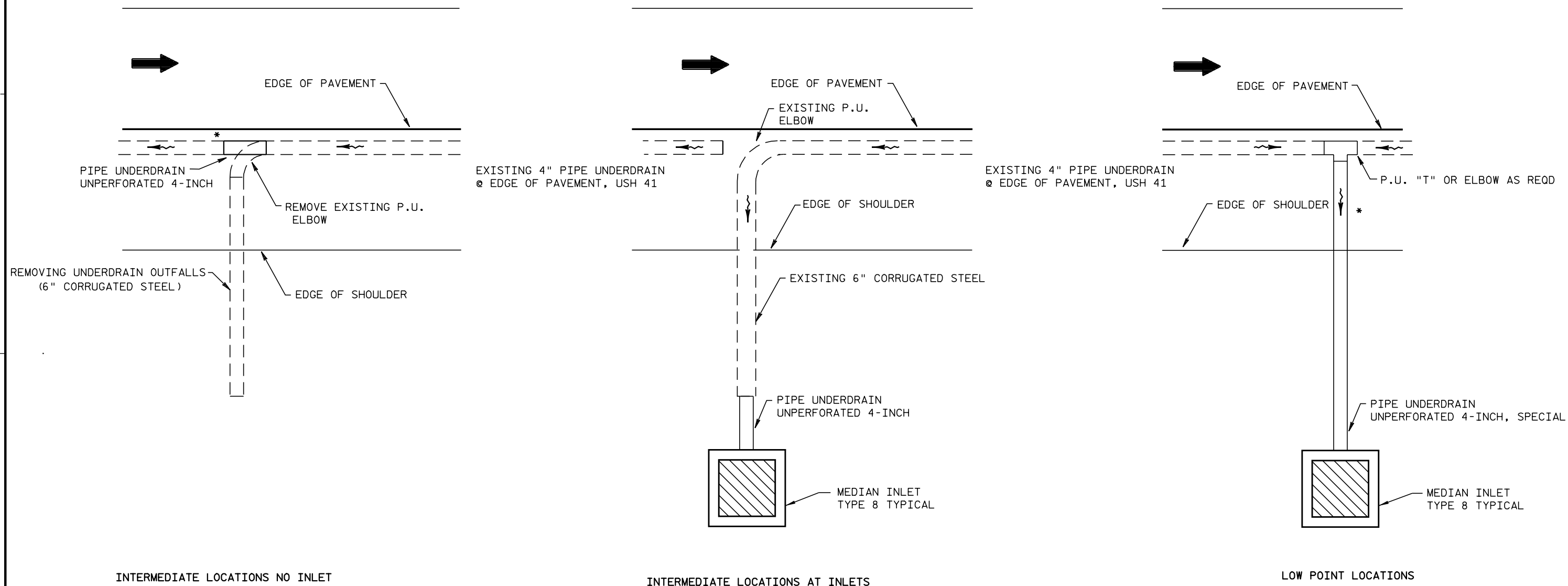
CONCRETE BARRIER, SINGLE FACE, TYPE F, 51-INCH

A - A

CONCRETE BARRIER TRANSITION  
 TO THRIE BEAM  
 S.D.D. 14B22-3B



SIDE VIEW



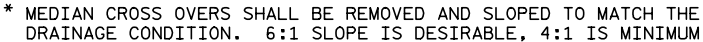
### DETAIL FOR EXTENSION OF UNDERDRAIN OUTFALLS

(SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS)

\* SPECIAL GRANULAR BACKFILL REQD.  
UNDER PAVED SHOULDER

2

2

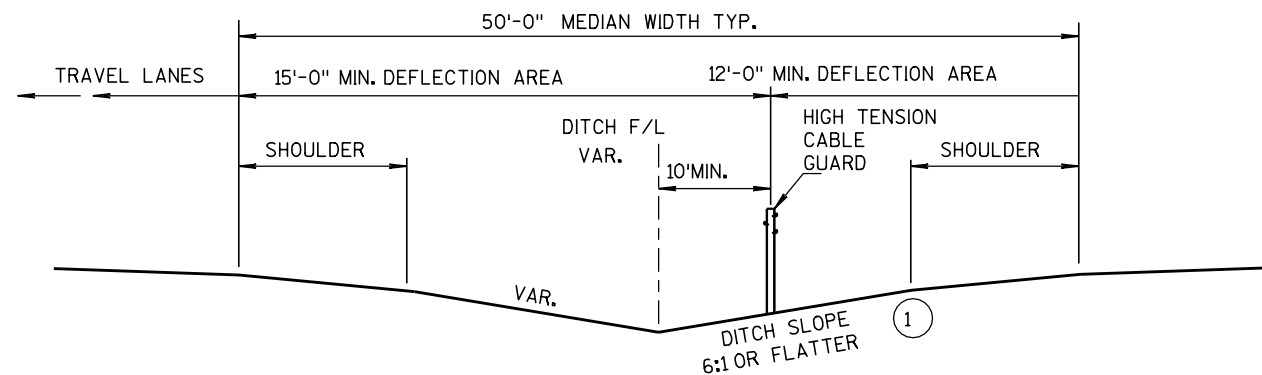


TYPICAL MEDIAN/MAINTENANCE CROSSOVER REMOVAL DETAIL - STAGE 2

FOR LOCATIONS SEE PLAN SHEETS  
STA. 1904+75



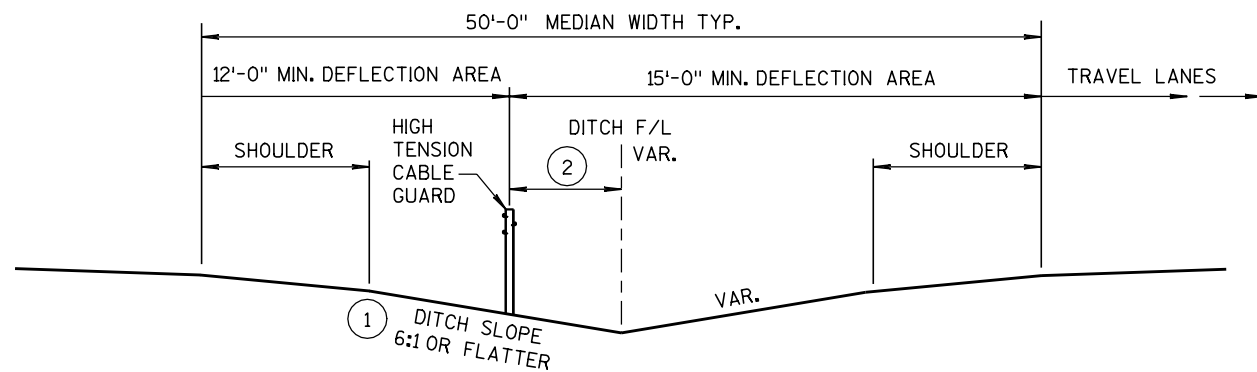
|              |              |
|--------------|--------------|
| STA. 1816+30 | STA. 2064+00 |
| STA. 1963+70 | STA. 2097+50 |
| STA. 2043+05 |              |



### CABLE BARRIER OFFSET LOCATIONS

BARRIER 4-STRAND HT CABLE:

STA. 1879+00 - 1887+00  
STA. 1974+00 - 1980+00



### CABLE BARRIER OFFSET LOCATIONS

(LEFT OF DITCH F/L)

BARRIER 4-STRAND HT CABLE:

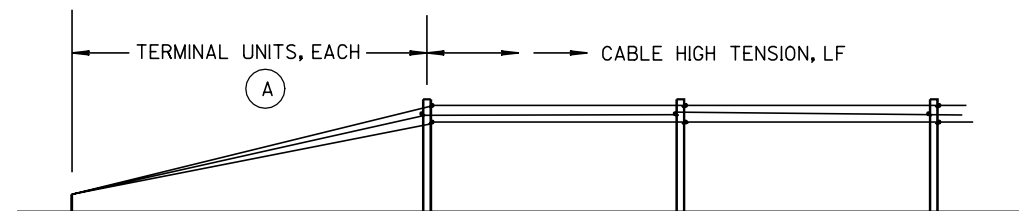
STA. 1804+50 - 1830+50  
STA. 1865+00 - 1879+00  
STA. 1911+90 - 1974+00  
STA. 1980+00 - 2015+80

BARRIER 3-STRAND HT CABLE:

STA. 2018+00 - 2076+00  
STA. 2097+70 - 2160+25

### GENERAL NOTES

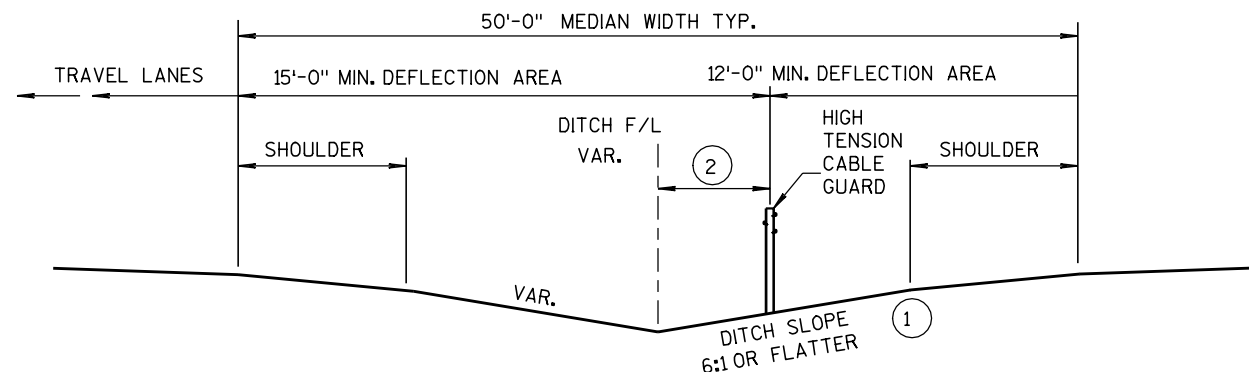
- DO NOT INSTALL CABLE GUARD BARRIER ON SLOPES STEEPER THAN 6:1.
- OFFSET CABLE GUARD RAIL TO EITHER SIDE OF MEDIAN CENERLINE:  
USE 1'-0" MIN. OFFSET FOR MEDIAN 50 FEET



- (A) CABLE HIGH TENSION THAT FALLS WITHIN THE TERMINAL UNIT IS INCLUDED IN COST OF TERMINAL UNIT.

TERMINAL UNITS, EACH

BRIFFEN WRGT END TERMINAL = 25'-6"  
CASS CCT TERMINAL = 51'-3".



### CABLE BARRIER OFFSET LOCATIONS

(RIGHT OF DITCH F/L)

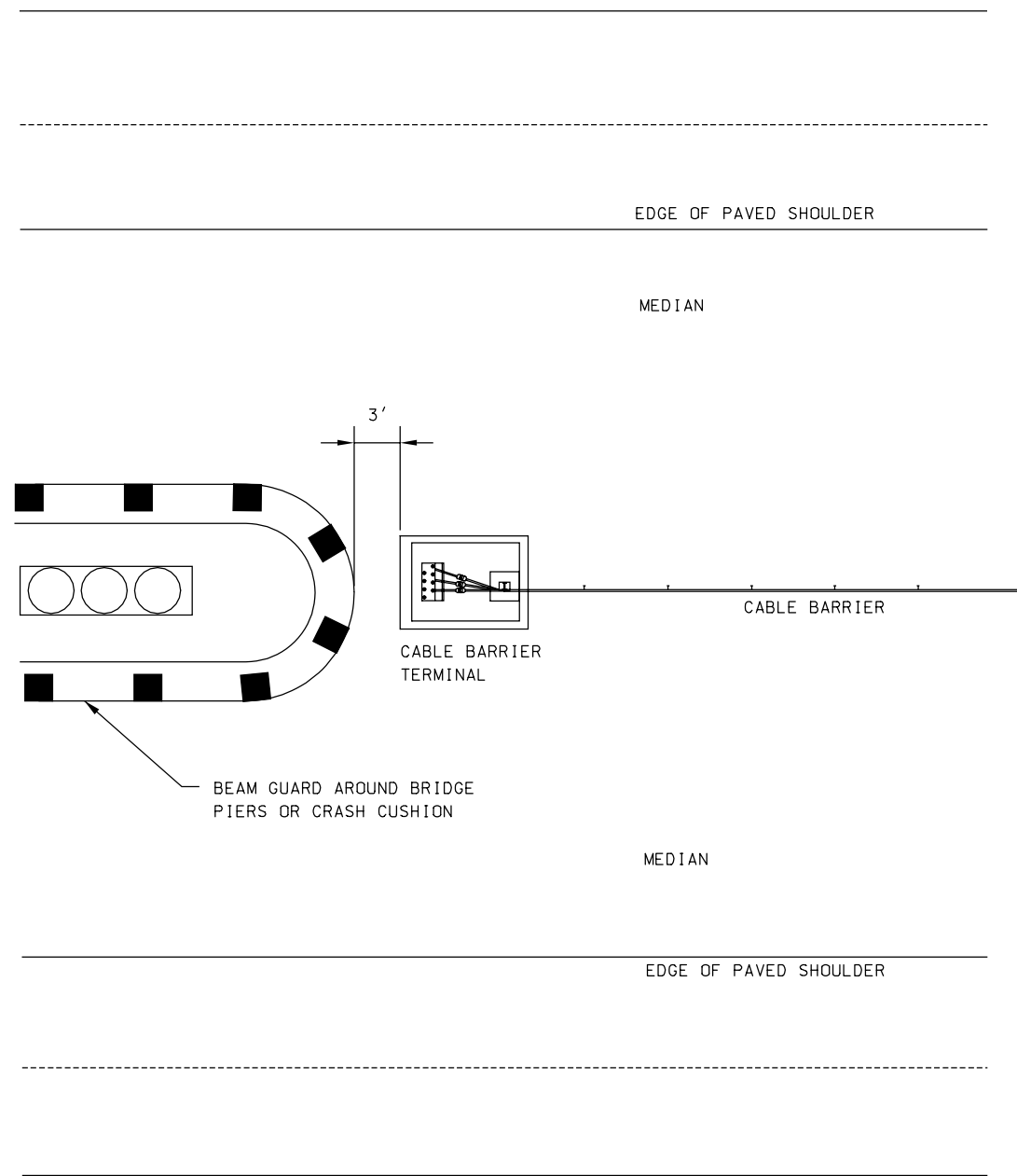
BARRIER 4-STRAND HT CABLE:

STA. 1832+00 - 1865+00  
STA. 1887+00 - 1909+10

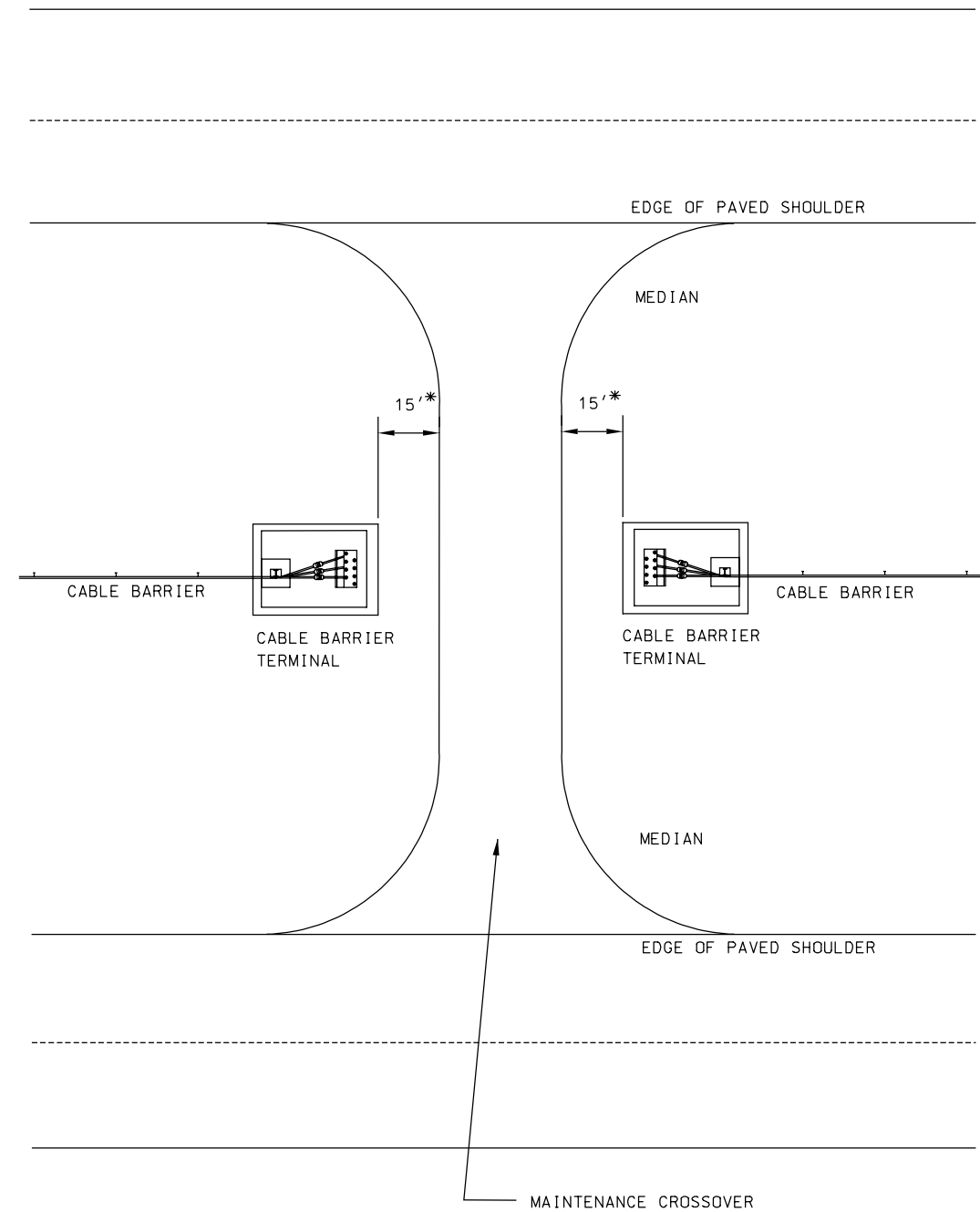
BARRIER 3-STRAND HT CABLE:

STA. 2076+00 - 2095+70

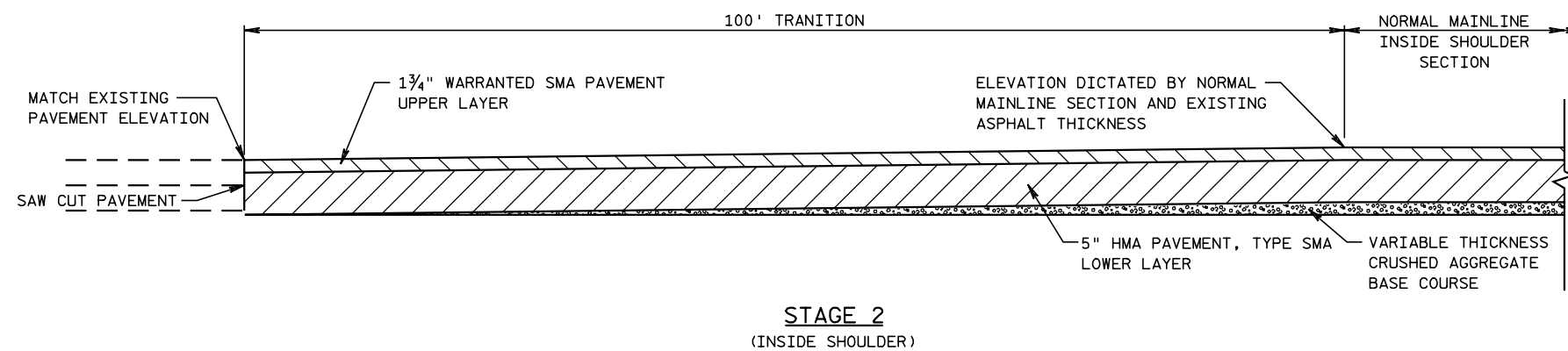
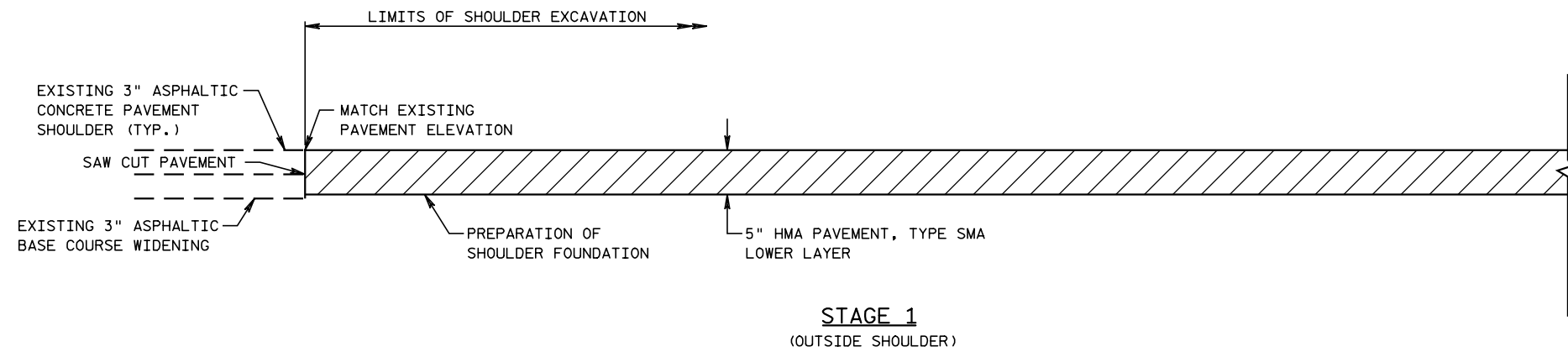
CABLE BARRIER TERMINAL END  
DETAIL A



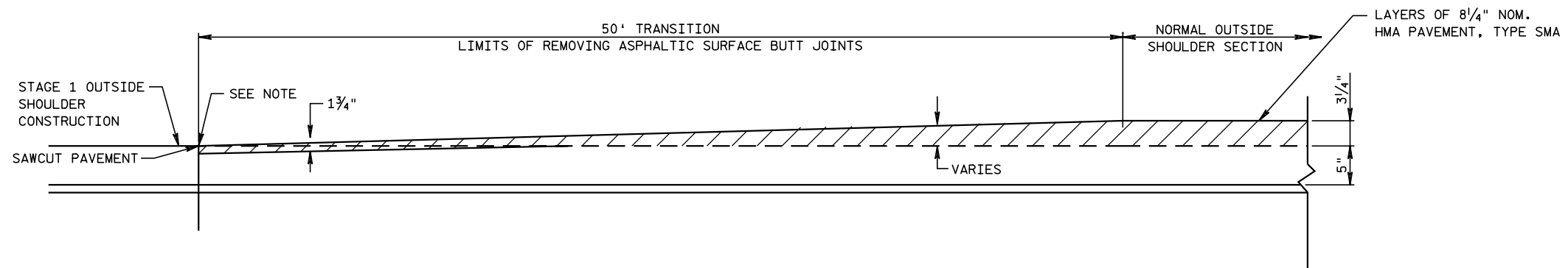
CABLE BARRIER TERMINAL END  
DETAIL B



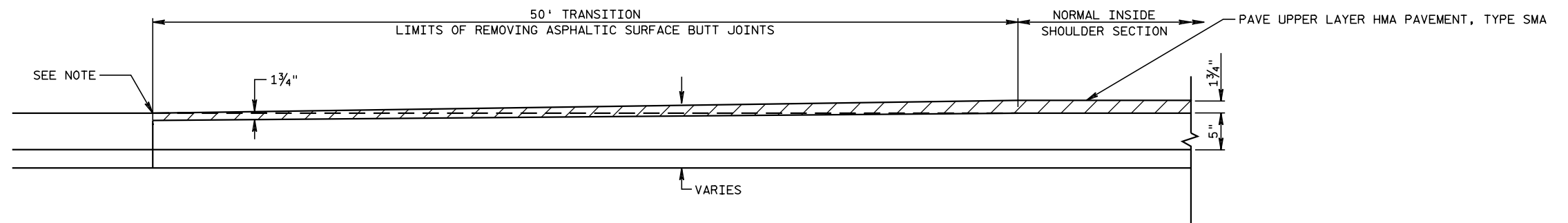
\* - DISTANCE MEASURED FROM  
EDGE OF ASPHALT PAVEMENT



SHOULDER DETAILS FOR BUTT JOINTS-TYPICAL

**NOTE:**

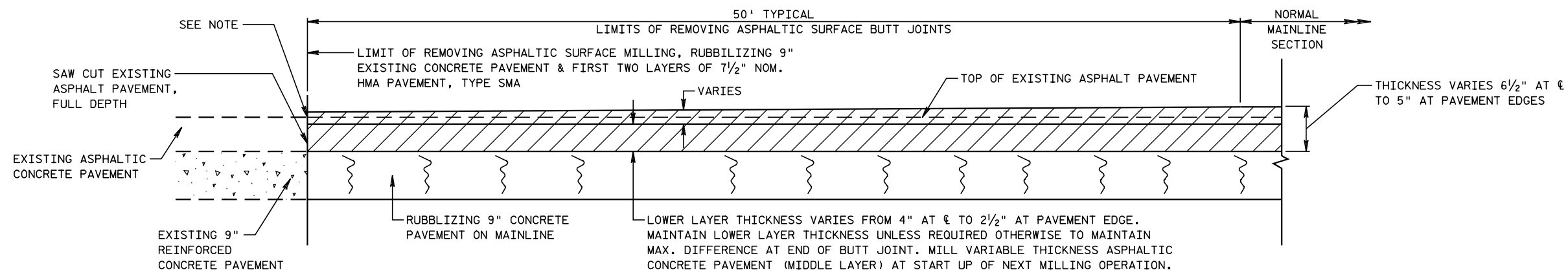
BUTT JOINTS AT BEGIN/END OF PROJECT SHALL MATCH EXISTING PAVEMENT ELEVATION.

**TYPICAL BUTT JOINT DETAIL FOR  
STAGE 3 & 4A FOR OUTSIDE SHOULDER****NOTE:**

BEGIN/END OF PROJECT SHALL MATCH EXISTING PAVEMENT ELEVATION.

**TYPICAL BUTT JOINT DETAIL FOR  
STAGE 4B FOR INSIDE SHOULDER**

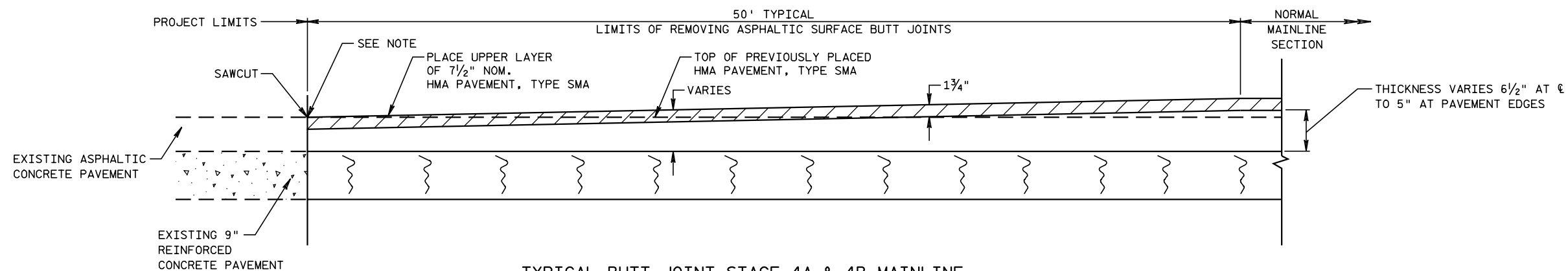
\*MILL VARIABLE THICKNESS ASPHALTIC CONCRETE PAVEMENT (MIDDLE LAYER) AT START UP OF NEXT MILLING OPERATION UNLESS AT PROJECT LIMITS.



TYPICAL BUTT JOINT FOR STAGE 2 & 3 MAINLINE

NOTE:

BUTT JOINTS AT BEGIN/END OF PROJECT SHALL MATCH EXISTING PAVEMENT ELEVATION.

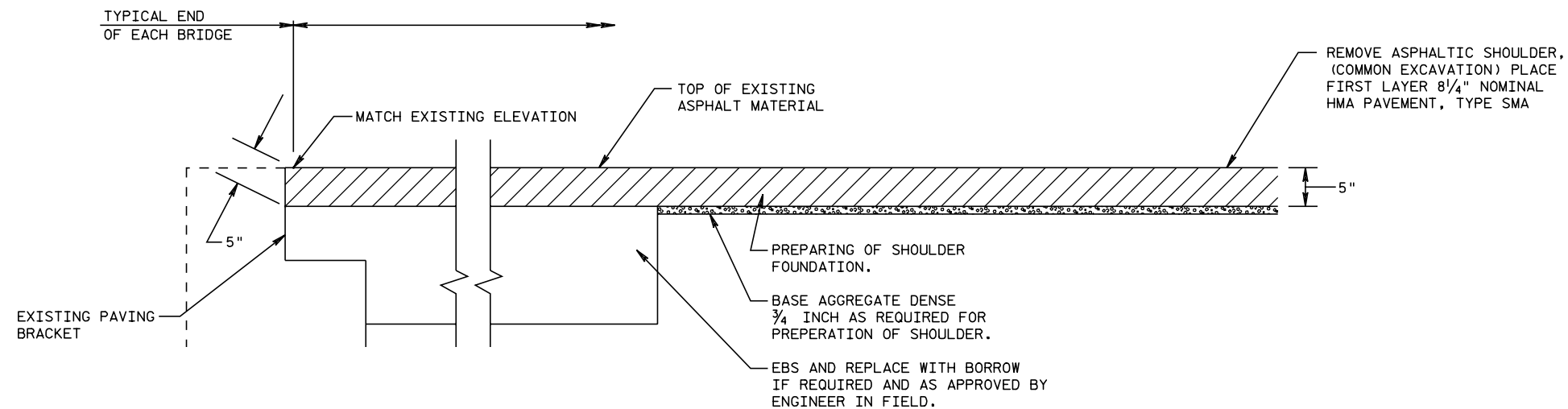


TYPICAL BUTT JOINT STAGE 4A & 4B MAINLINE

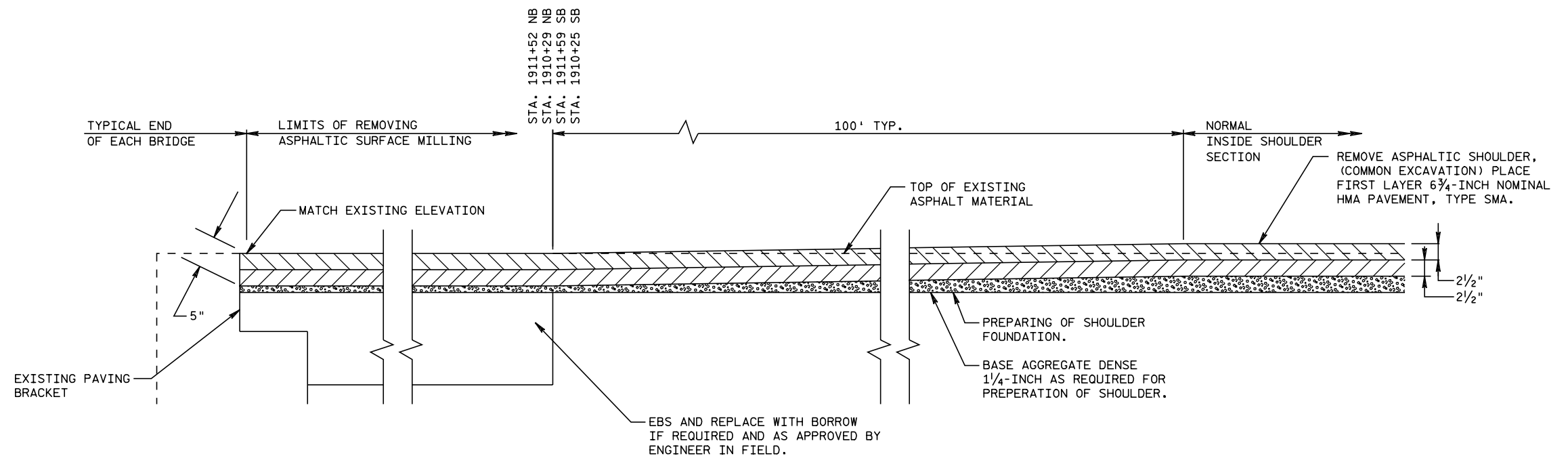
SHOWN AT PROJECT LIMITS LOCATION

NOTE:

BUTT JOINTS AT BEGIN/END OF PROJECT SHALL MATCH EXISTING PAVEMENT ELEVATION.

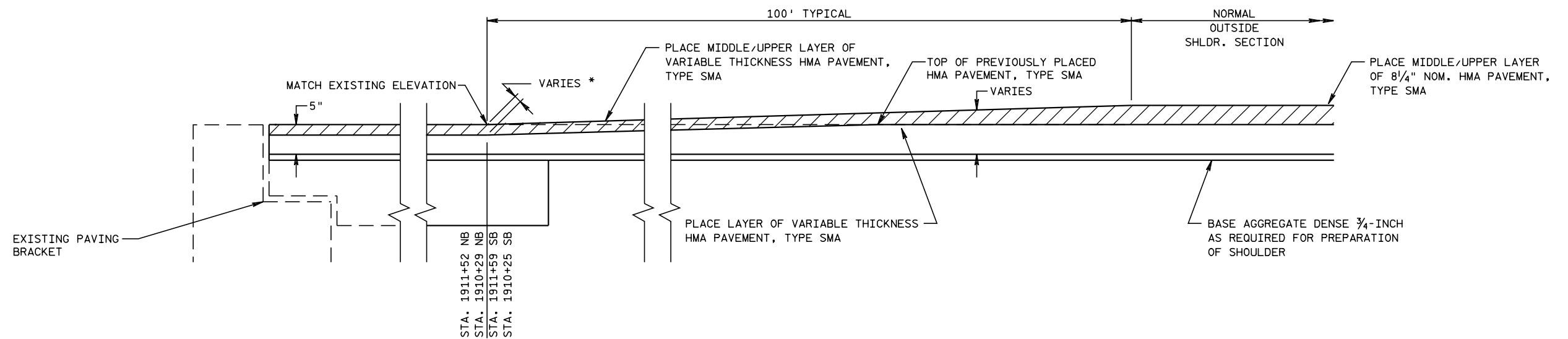


**B-20-09 & B-20-24 STRUCTURE APPROACH  
BUTT JOINT FOR STAGE 1 OUTSIDE SHOULDER**



**B-20-09 & B-20-24 STRUCTURE APPROACH  
BUTT JOINT FOR STAGE 2 INSIDE SHOULDER**

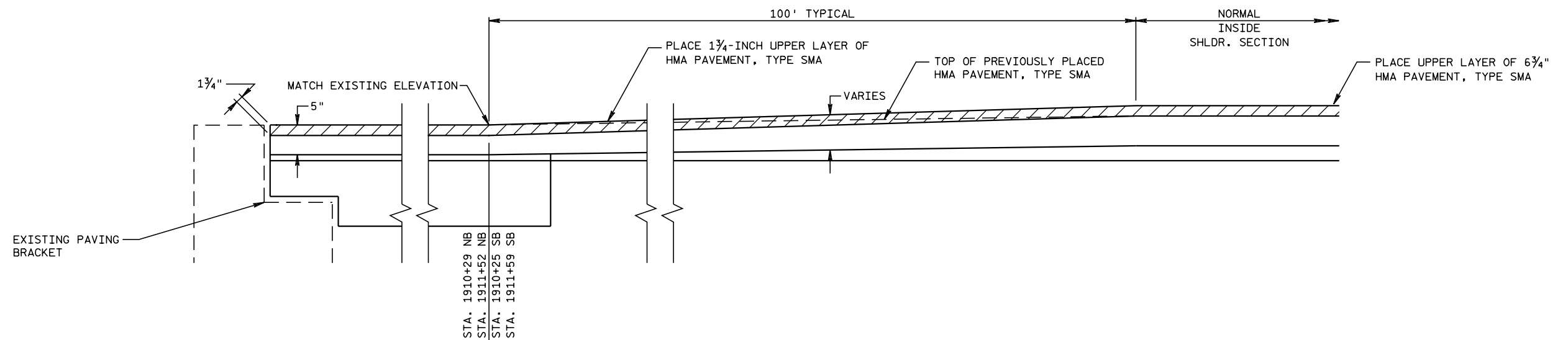
**NOTE:**  
SHOWN FOR NB LANES SOUTH APPROACH. SIMILAR FOR  
NB LANES NORTH APPROACH AND SB APPROACHS.



**B-20-09 & B-20-24 STRUCTURE APPROACH  
BUTT JOINT FOR STAGE 3 & 4A OUTSIDE SHOULDER**

**NOTE:**

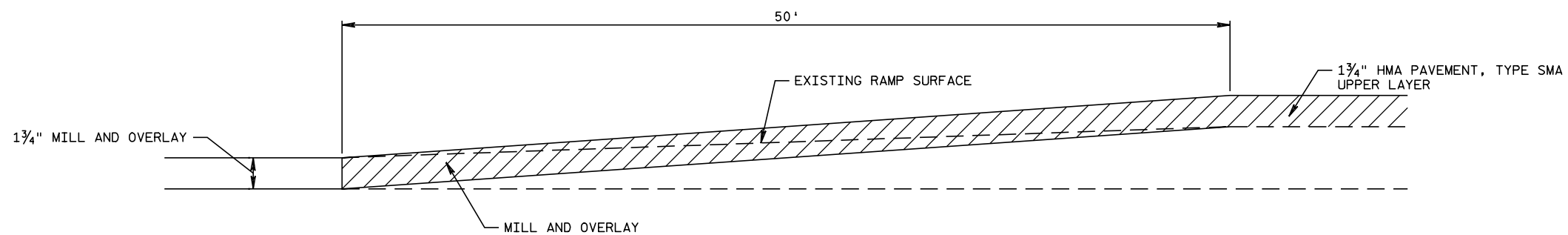
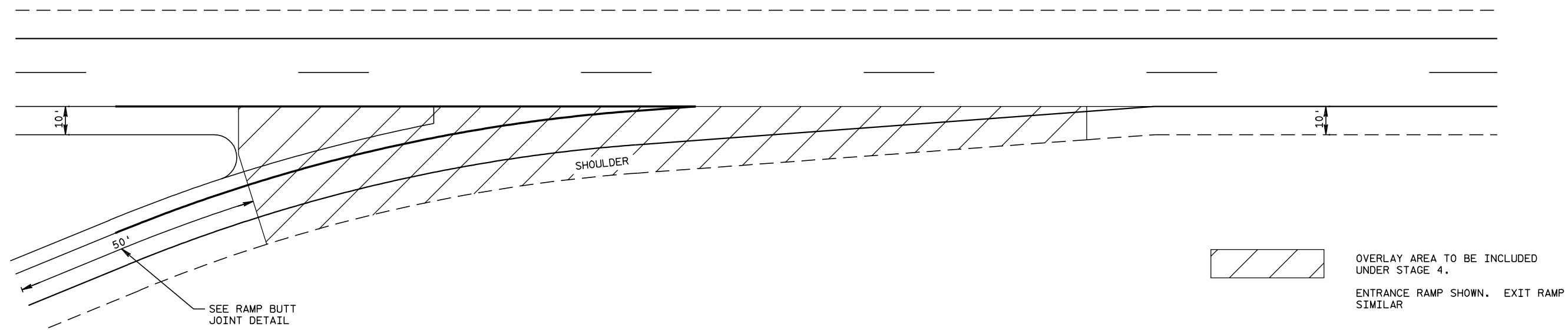
SHOWN FOR NB LANES SOUTH APPROACH. SIMILAR FOR NB LANES NORTH APPROACH AND SB APPROACHES.



**B-20-09 & B-20-24 STRUCTURE APPROACH  
BUTT JOINT FOR STAGE 4B INSIDE SHOULDER**

**NOTE:**

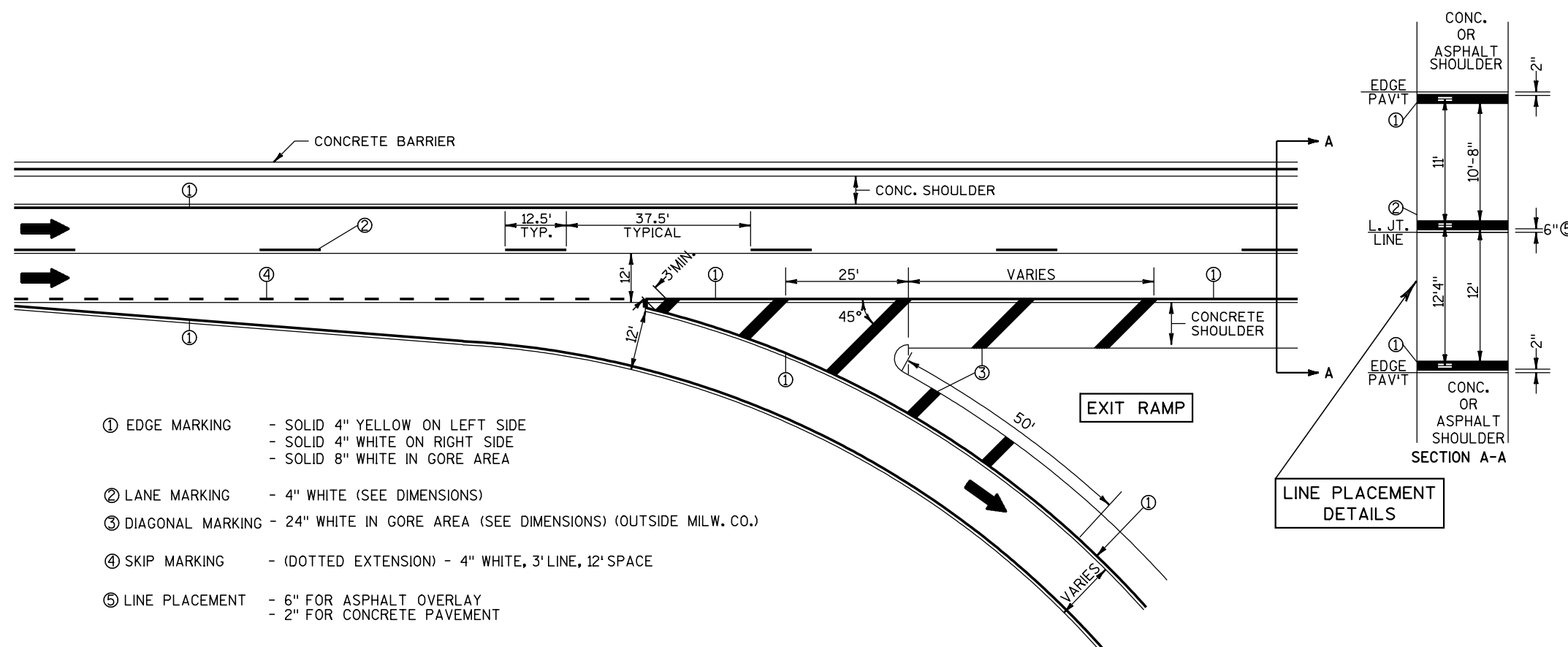
SHOWN FOR NB LANES SOUTH APPROACH. SIMILAR FOR NB LANES NORTH APPROACH AND SB APPROACHES.



STAGE 4A

RAMP BUTT JOINT DETAILS

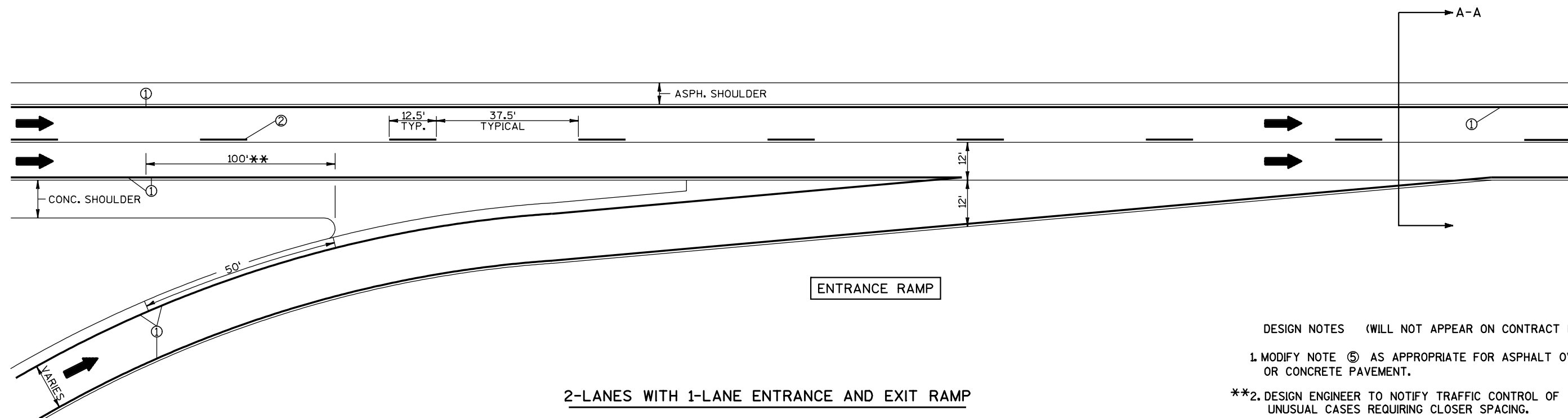
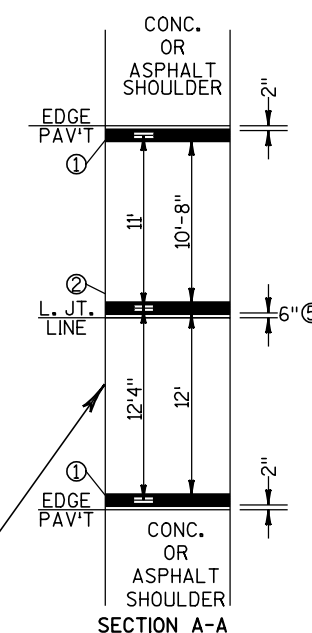
RACINE, KENOSHA, WALWORTH, WAUKESHA,  
FOND DU LAC, OZAUKEE & WASHINGTON  
COUNTIES



#### NOTES:

1. WHERE THE DOTTED EXTENSION OF THE RIGHT EDGE LINE IS SHOWN ON THE PLAN, IT SHALL BE 4" WIDE, WITH A LINE SKIP PATTERN CONSISTING OF A 3' LONG LINE AND A 12' SKIP.
2. PAVEMENT LANE MARKINGS SHALL BE LOCATED 6" OFF THE LONGITUDINAL JOINT OF THE UNDERLYING CONC. PAVEMENT.
3. ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
4. SPOTTING FOR PAVEMENT LANE MARKINGS AND EDGE LINES SHALL BE 4"x4" PIECES OF REFLECTORIZED PAV'T MARKING TAPE SPACED AT 50' INTERVALS, INCIDENTAL TO PAV'T MARKING.
5. SEE STANDARD DETAIL DRAWING FOR RAISED PAVEMENT MARKERS IN GORE AND LINE WIDTH TOLERANCES.

LINE PLACEMENT  
DETAILS



#### DESIGN NOTES (WILL NOT APPEAR ON CONTRACT PLANS)

1. MODIFY NOTE ⑤ AS APPROPRIATE FOR ASPHALT OVERLAY OR CONCRETE PAVEMENT.
- \*\*2. DESIGN ENGINEER TO NOTIFY TRAFFIC CONTROL OF UNUSUAL CASES REQUIRING CLOSER SPACING.

2-LANES WITH 1-LANE ENTRANCE AND EXIT RAMP

GORES, FREEWAYS, & EXPRESSWAYS

PROJECT NO: 1104-12-70

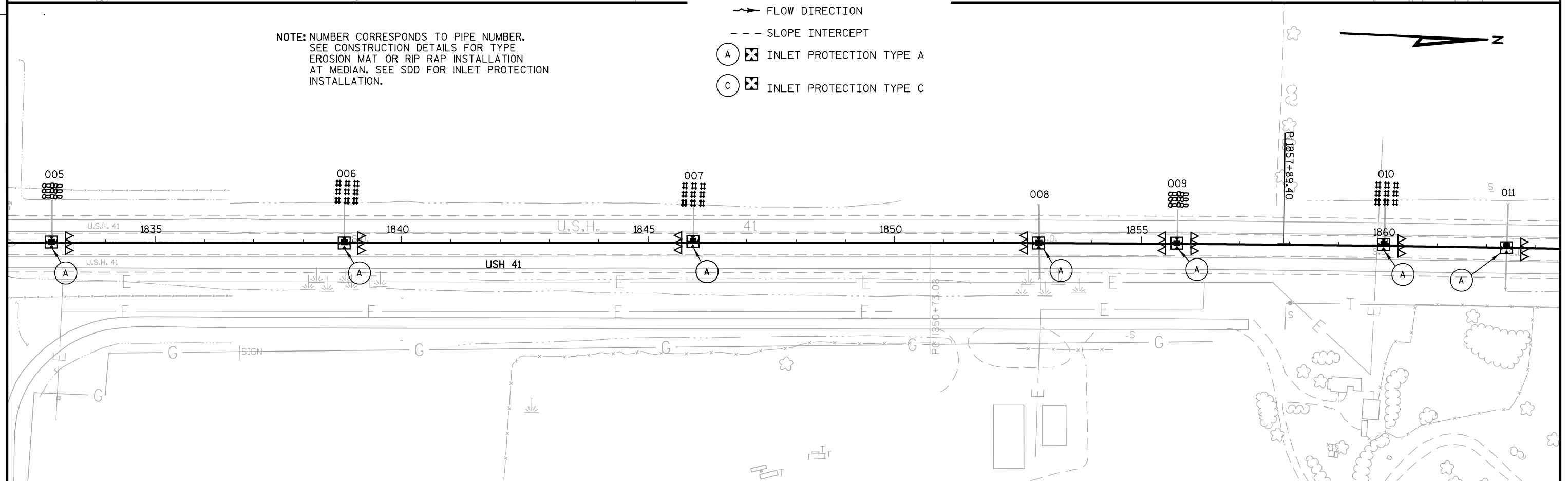
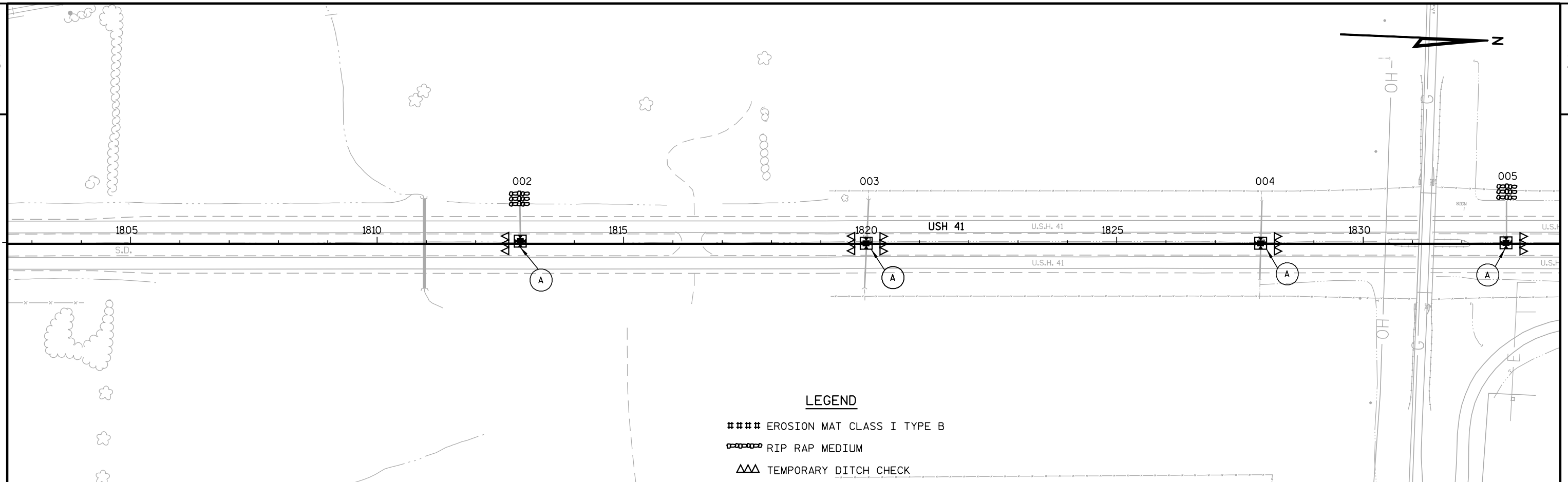
HWY: USH 41

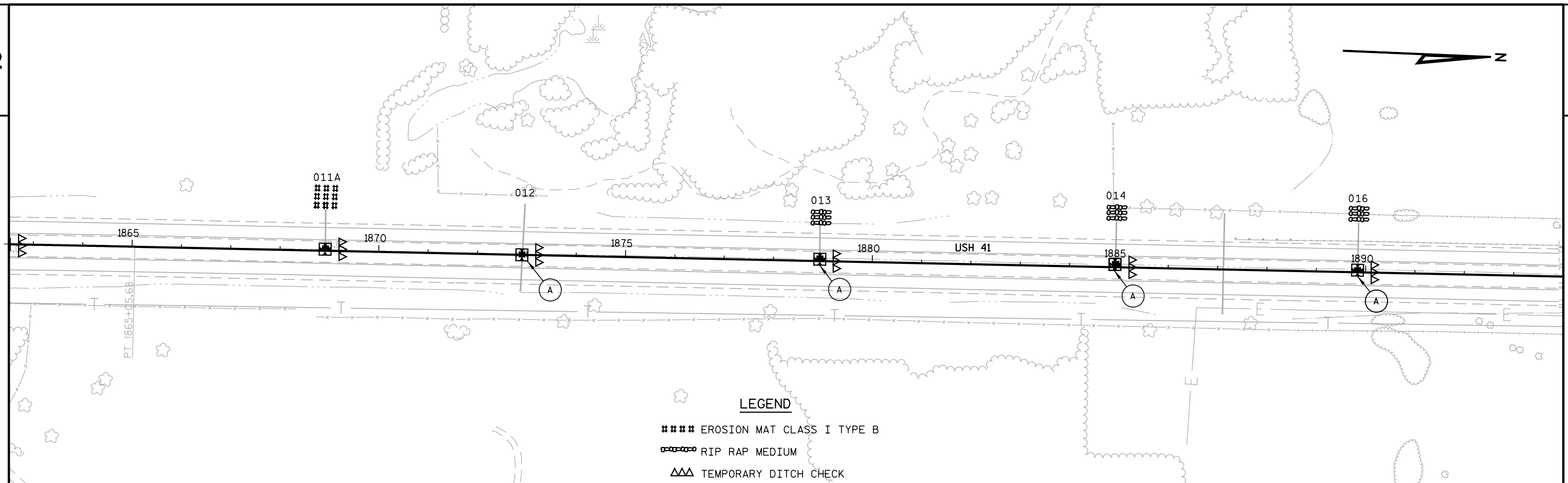
COUNTY: FOND DU LAC

EPOXY PAVEMENT MARKINGS, RAMPS

SHEET

E



**LEGEND**

#### EROSION MAT CLASS I TYPE B

-o-o-o- RIP RAP MEDIUM

△△△ TEMPORARY DITCH CHECK

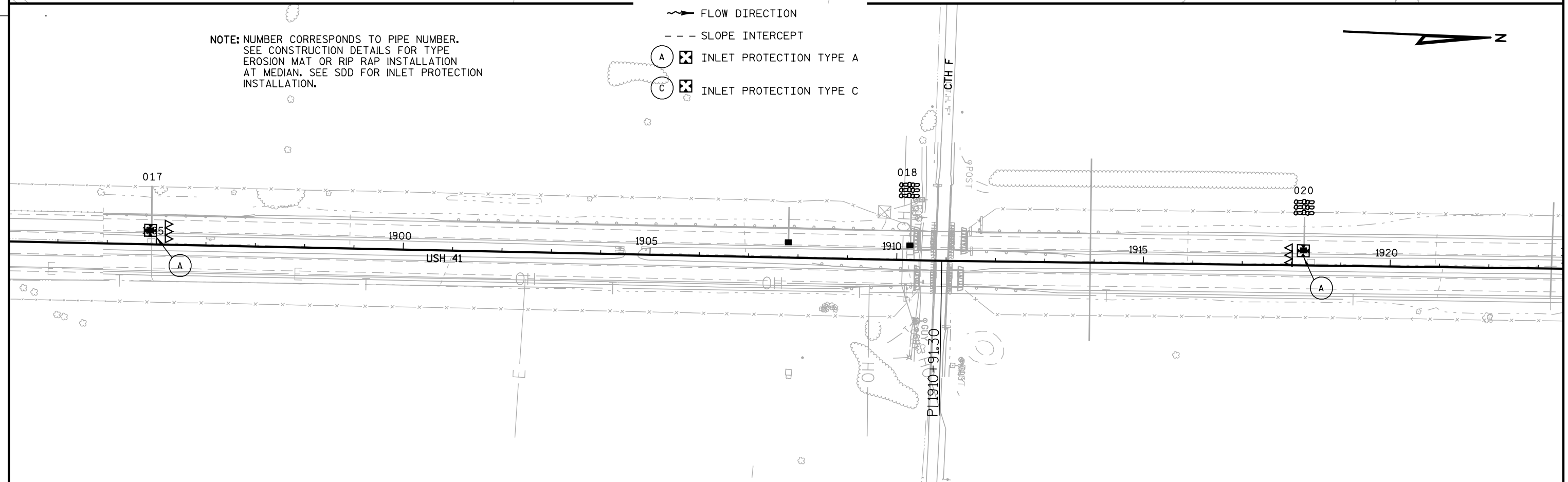
~&gt; FLOW DIRECTION

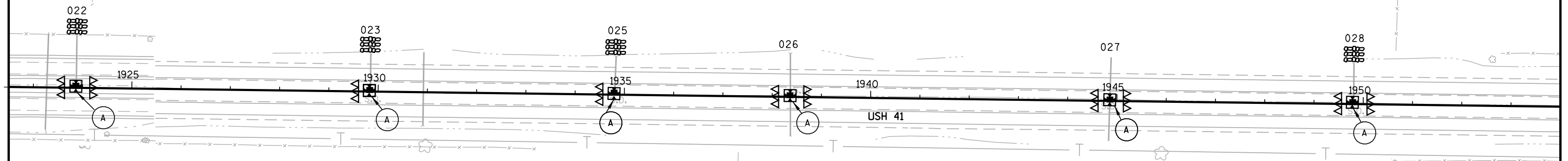
--- SLOPE INTERCEPT

(A) ⊠ INLET PROTECTION TYPE A

(C) ⊠ INLET PROTECTION TYPE C

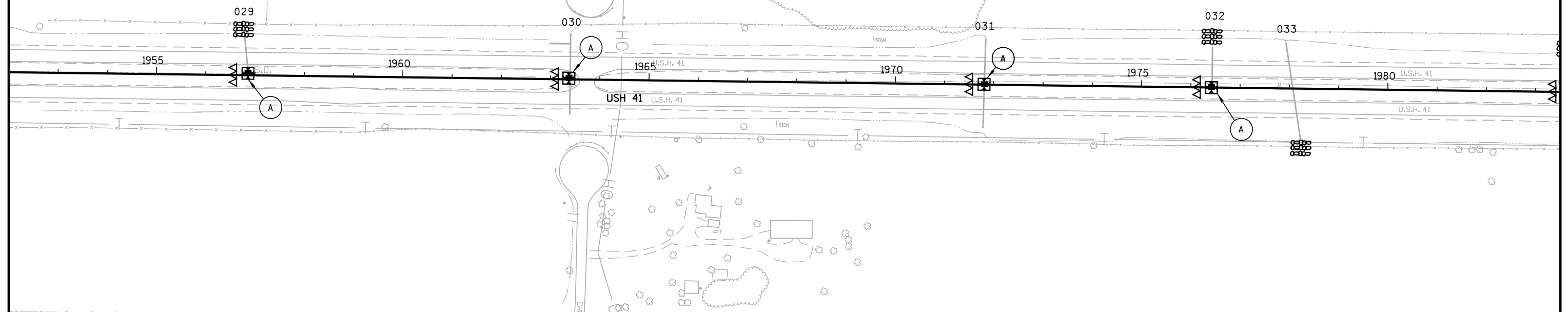
NOTE: NUMBER CORRESPONDS TO PIPE NUMBER.  
SEE CONSTRUCTION DETAILS FOR TYPE  
EROSION MAT OR RIP RAP INSTALLATION  
AT MEDIAN. SEE SDD FOR INLET PROTECTION  
INSTALLATION.

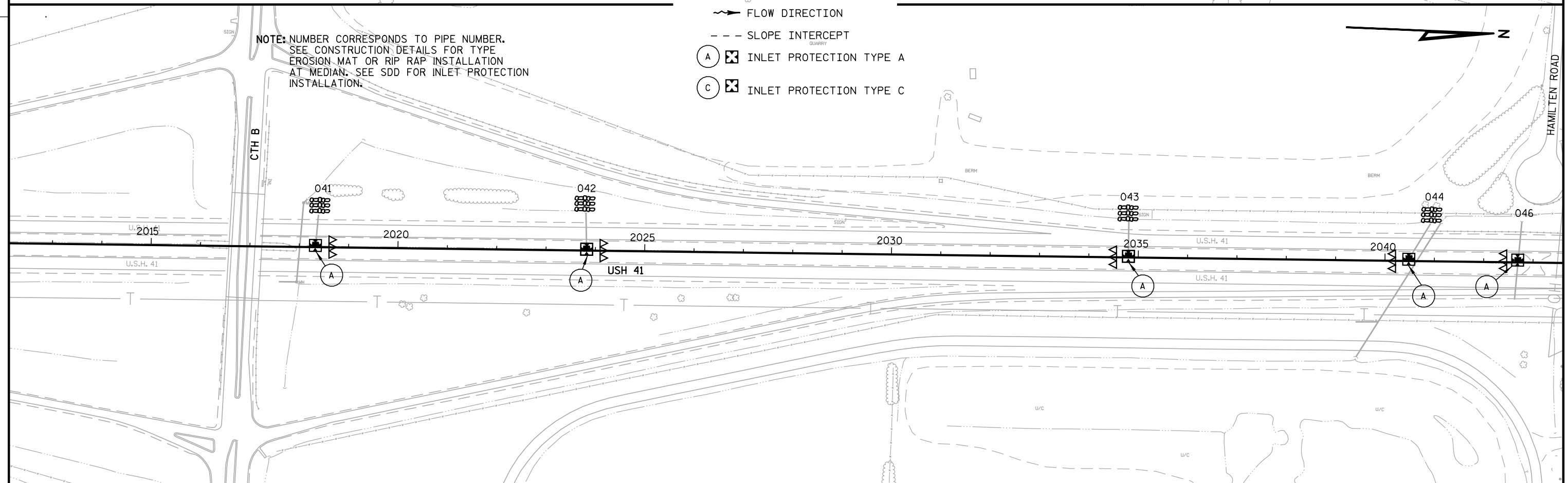
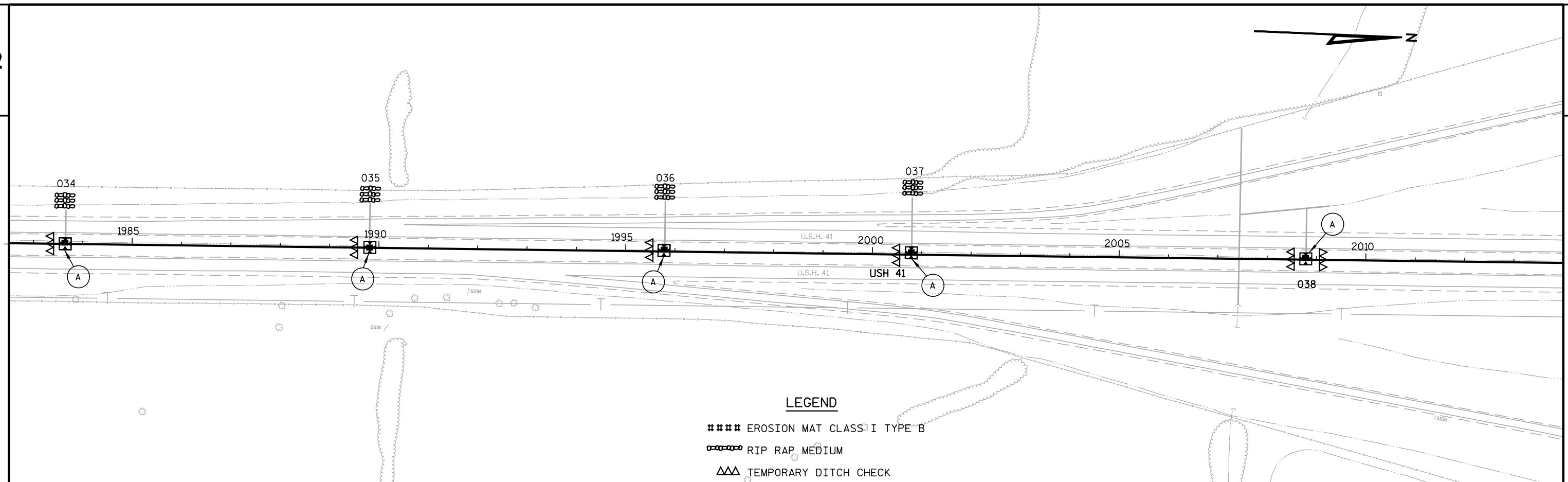


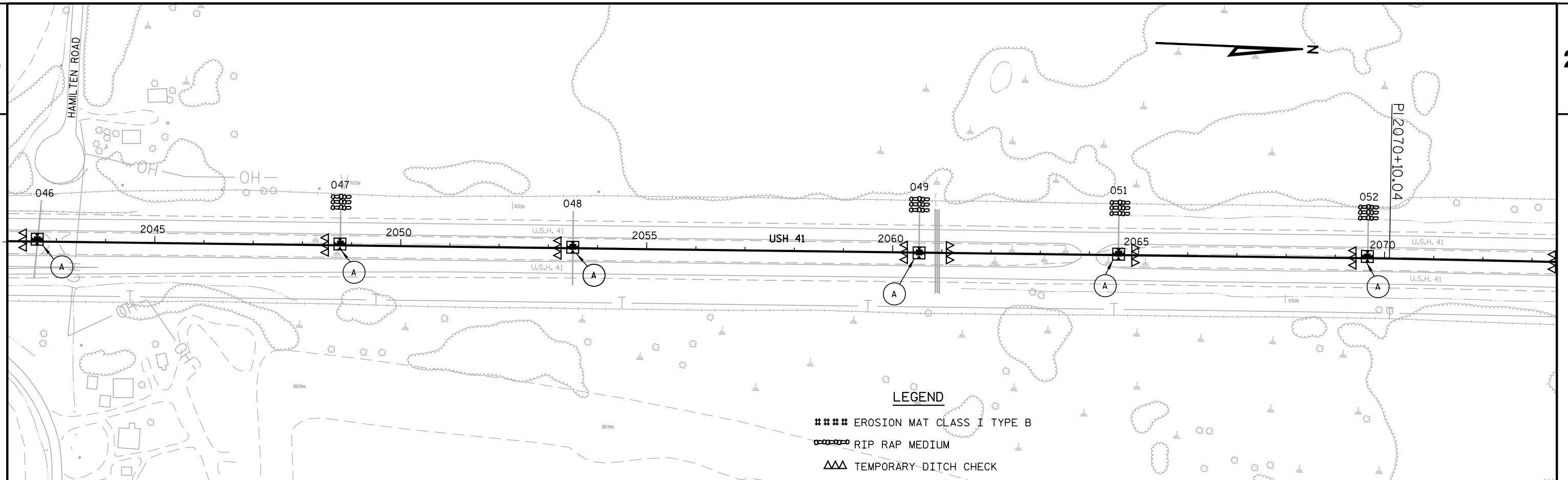
**LEGEND**

- #### EROSION MAT CLASS I TYPE B
- o---o---o--- RIP RAP MEDIUM
- \ \ /--- TEMPORARY DITCH CHECK
- > FLOW DIRECTION
- SLOPE INTERCEPT
- (A) [Symbol] INLET PROTECTION TYPE A
- (C) [Symbol] INLET PROTECTION TYPE C

NOTE: NUMBER CORRESPONDS TO PIPE NUMBER.  
SEE CONSTRUCTION DETAILS FOR TYPE  
EROSION MAT OR RIP RAP INSTALLATION  
AT MEDIAN. SEE SDD FOR INLET PROTECTION  
INSTALLATION.





**LEGEND**

#### EROSION MAT CLASS I TYPE B

- - - - RIP RAP MEDIUM

△△ TEMPORARY DITCH CHECK

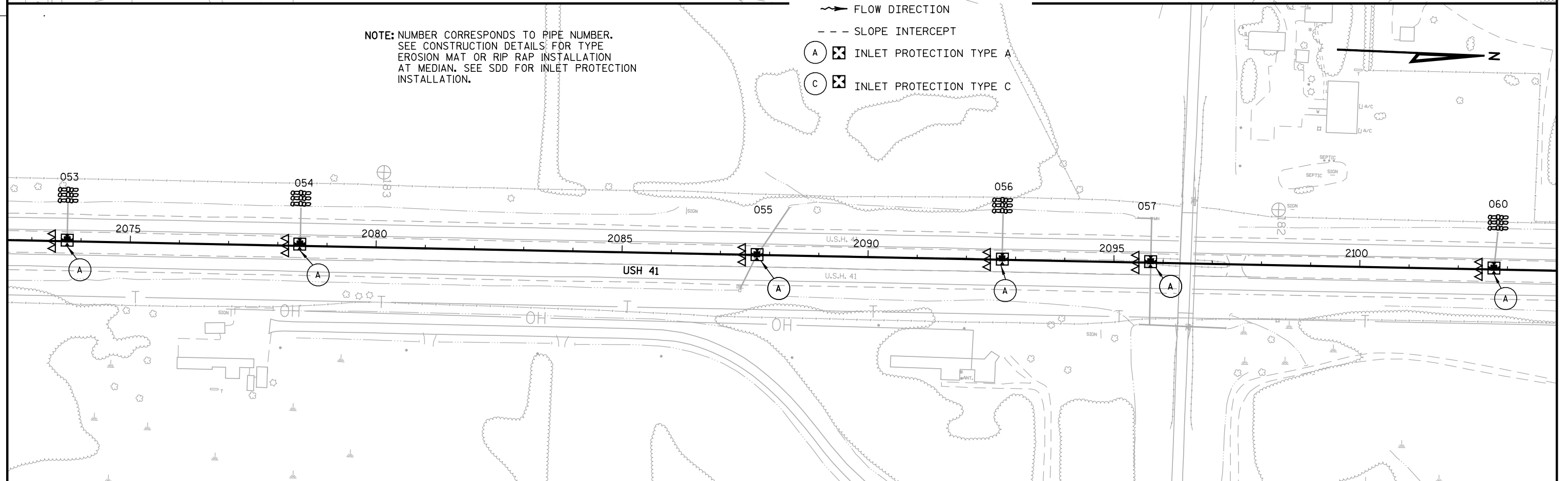
~&gt; FLOW DIRECTION

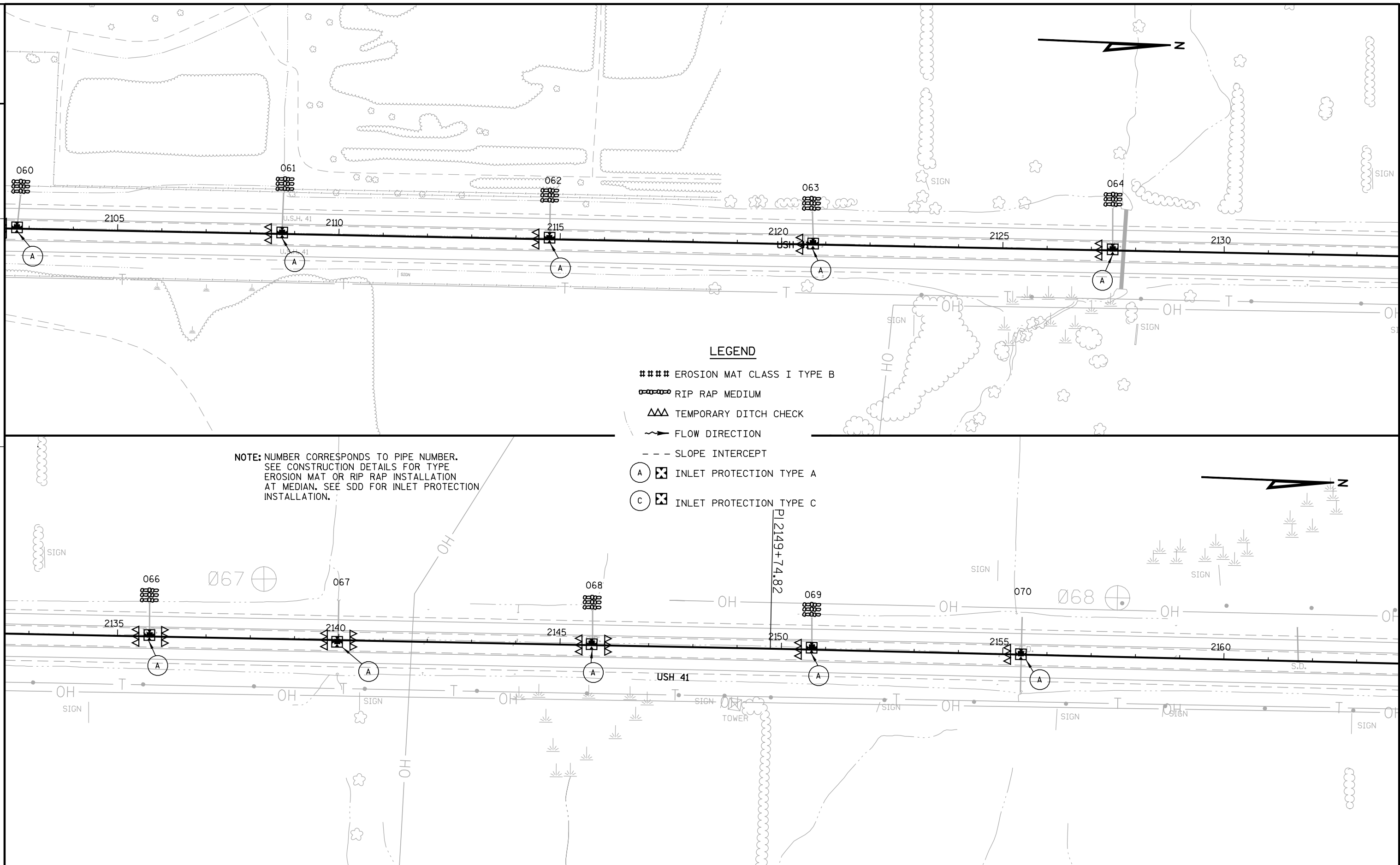
- - - SLOPE INTERCEPT

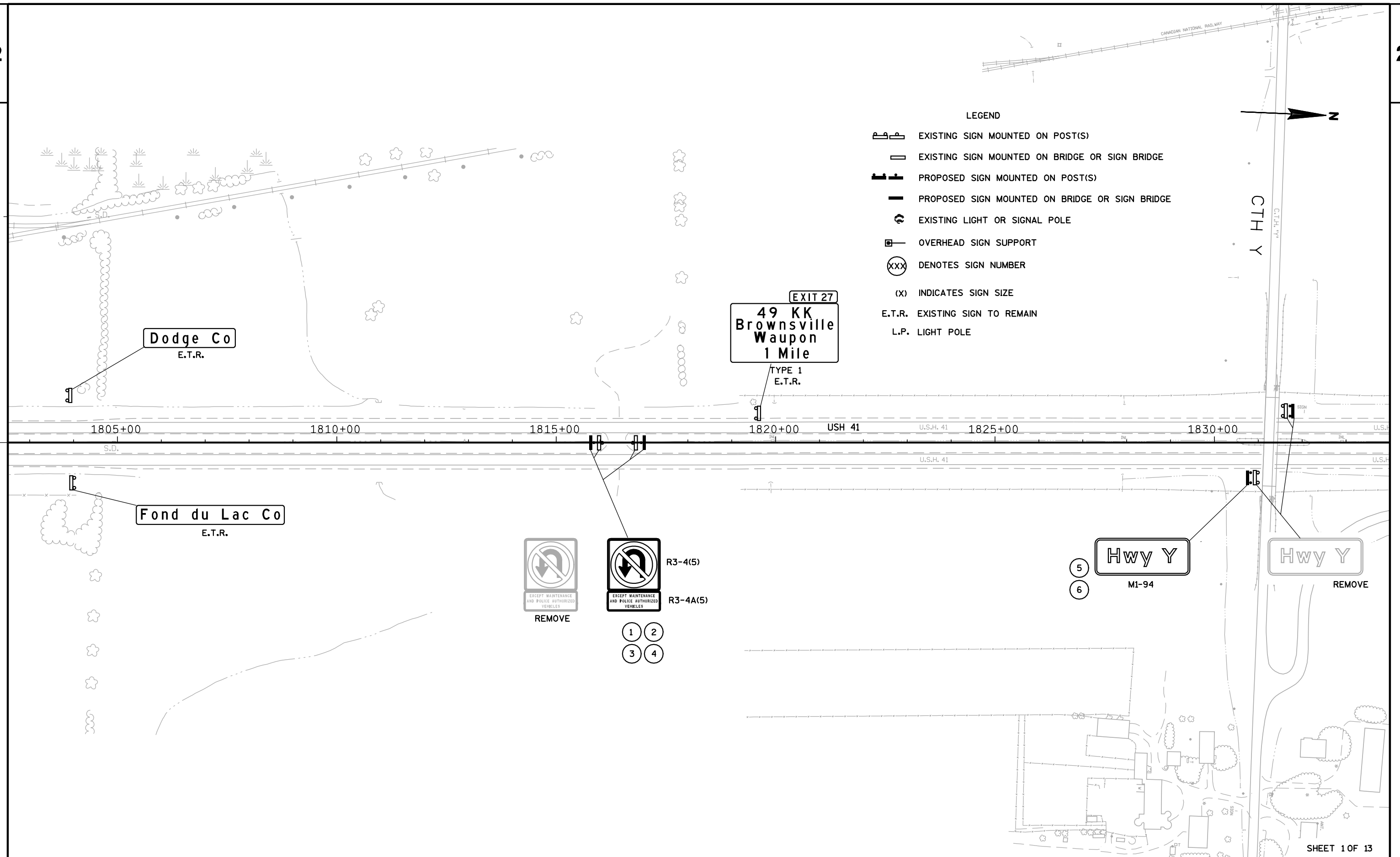
(A) [X] INLET PROTECTION TYPE A

(C) [X] INLET PROTECTION TYPE C







NOTE: NUMBER CORRESPONDS TO PIPE NUMBER.  
SEE CONSTRUCTION DETAILS FOR TYPE  
EROSION MAT OR RIP RAP INSTALLATION  
AT MEDIAN. SEE SDD FOR INLET PROTECTION  
INSTALLATION.







## LEGEND

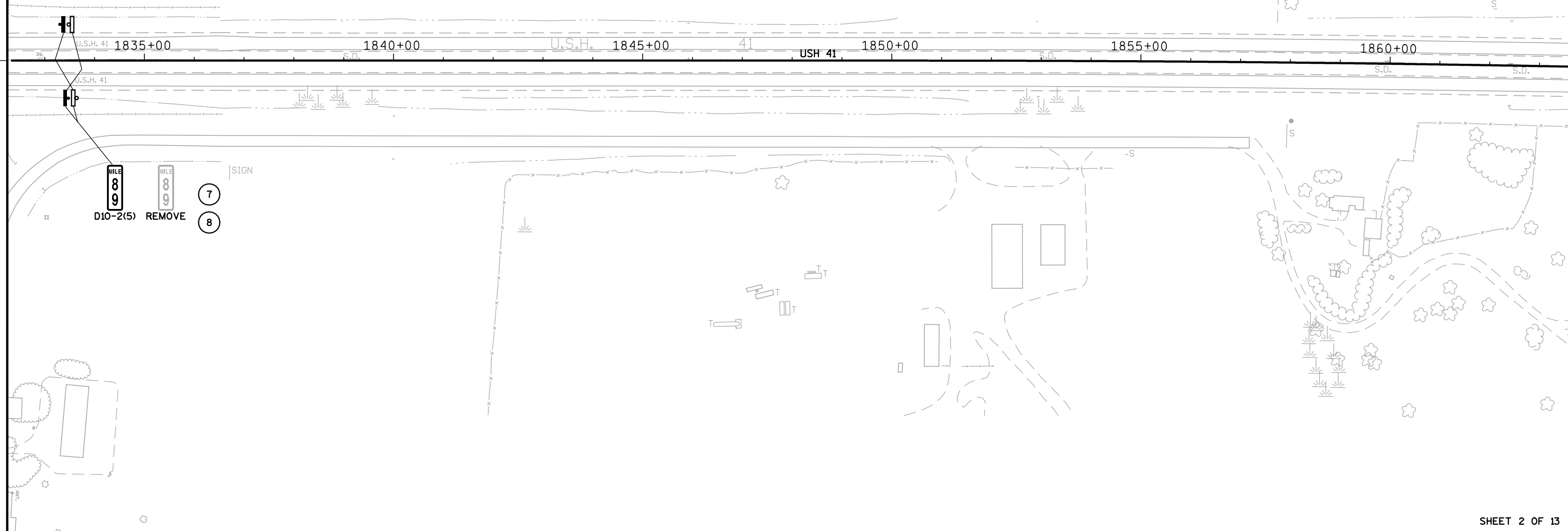
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 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 PROPOSED SIGN MOUNTED ON POST(S)  
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 EXISTING LIGHT OR SIGNAL POLE  
 OVERHEAD SIGN SUPPORT

DENOTES SIGN NUMBER

(X) INDICATES SIGN SIZE

E.T.R. EXISTING SIGN TO REMAIN

L.P. LIGHT POLE



SHEET 2 OF 13

PROJECT NO:1104-12-70

HWY: USH 41








COUNTY:FOND DU LAC

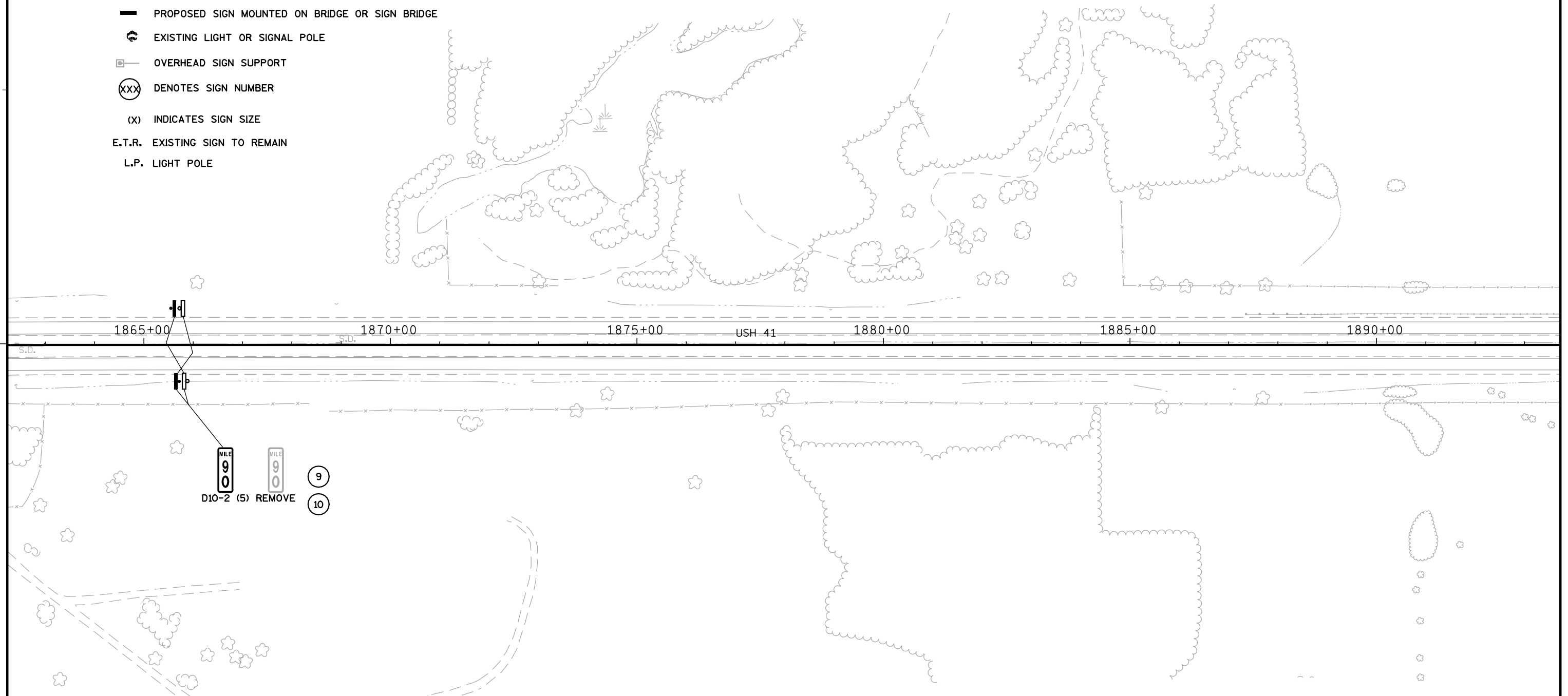
PERMANENT SIGNING

SHEET








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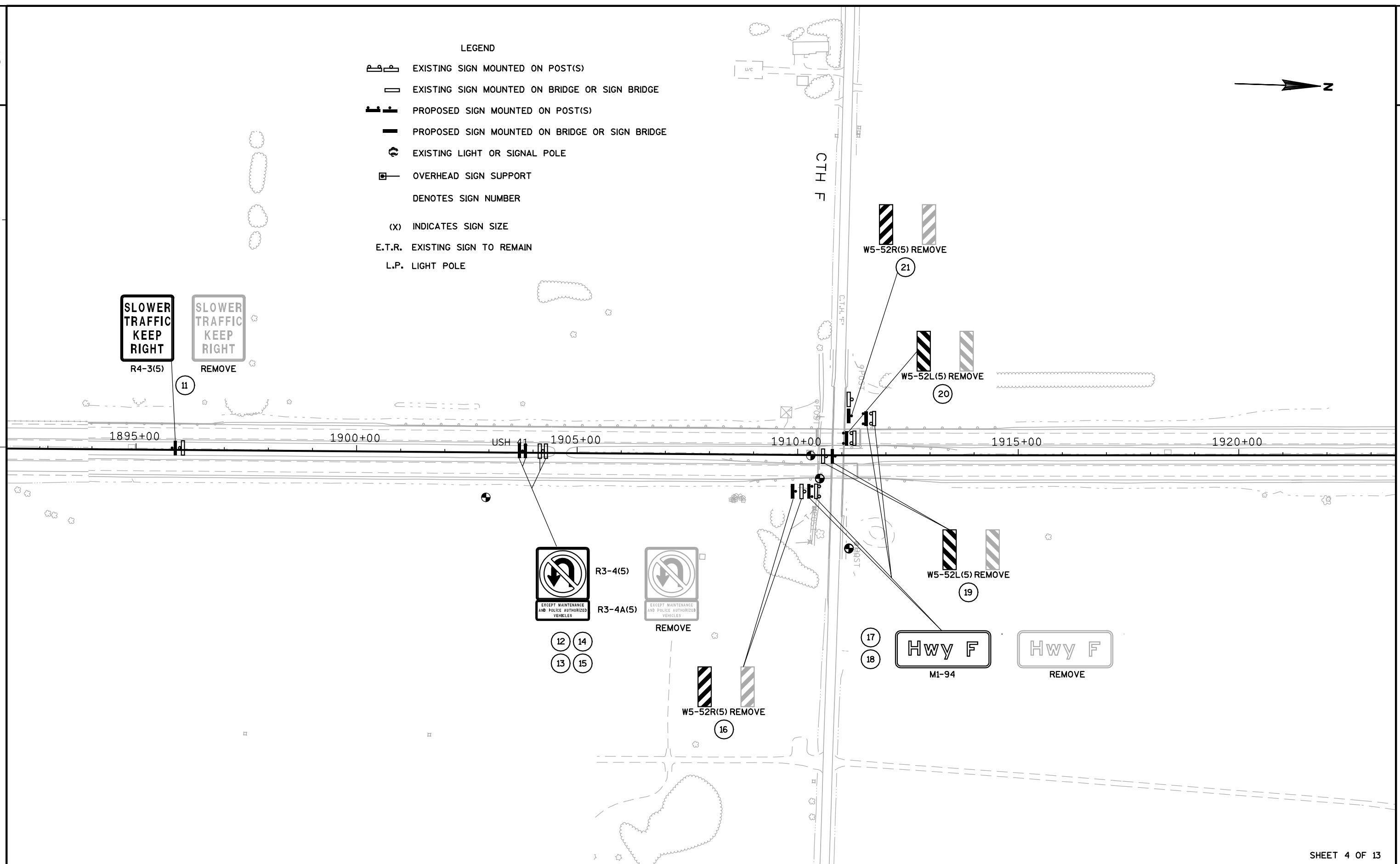
## LEGEND

-  EXISTING SIGN MOUNTED ON POST(S)  
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 PROPOSED SIGN MOUNTED ON POST(S)  
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 EXISTING LIGHT OR SIGNAL POLE  
 OVERHEAD SIGN SUPPORT  
 DENOTES SIGN NUMBER  
(X) INDICATES SIGN SIZE  
E.T.R. EXISTING SIGN TO REMAIN  
L.P. LIGHT POLE



## LEGEND

-  EXISTING SIGN MOUNTED ON POST(S)  
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 PROPOSED SIGN MOUNTED ON POST(S)  
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 EXISTING LIGHT OR SIGNAL POLE  
 OVERHEAD SIGN SUPPORT  
 DENOTES SIGN NUMBER  
  
(X) INDICATES SIGN SIZE  
E.T.R. EXISTING SIGN TO REMAIN  
L.P. LIGHT POLE



SHEET 4 OF 13

PROJECT NO:1104-12-70

HWY: USH 41

COUNTY:FOND DU LAC

PERMANENT SIGNING

SHEET

E

FILE NAME : J:\projects\02\_11041241\70\023204\_ps.dgn

PLOT DATE : 27-JUL-2005 11:16








PLOT BY : DOTR3H

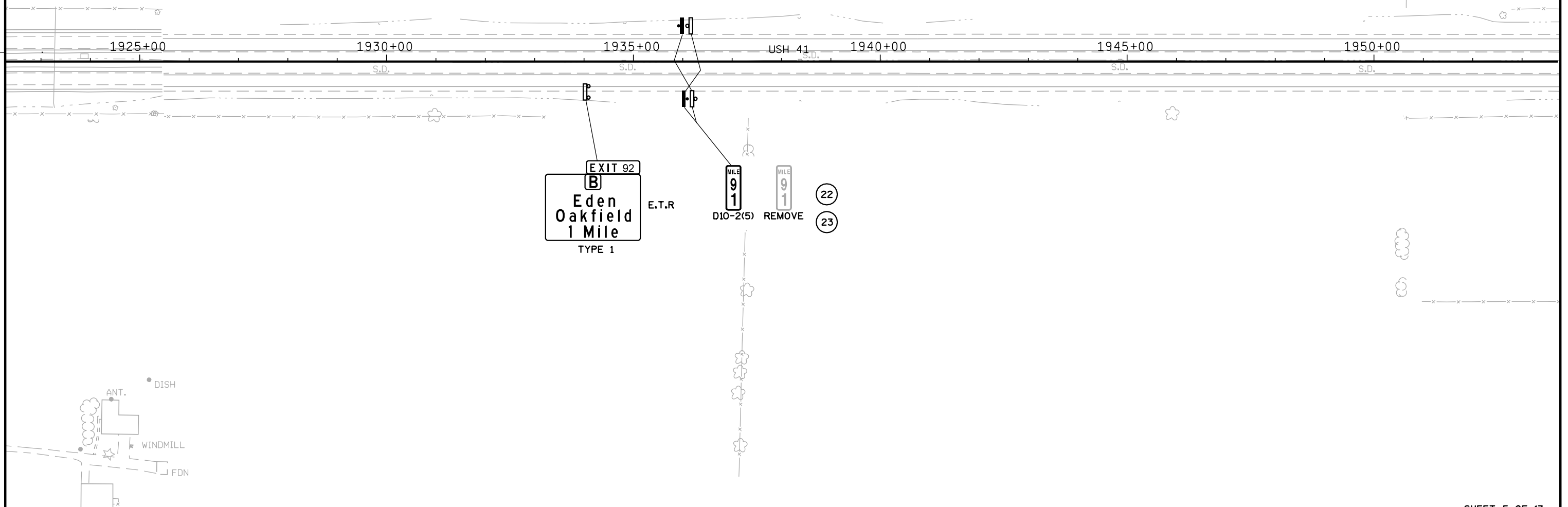
PLOT NAME :

PLOT SCALE : 199.999999:1.000000

WISDOT/CADDs SHEET 42

## LEGEND

-  EXISTING SIGN MOUNTED ON POST(S)  
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 PROPOSED SIGN MOUNTED ON POST(S)  
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 EXISTING LIGHT OR SIGNAL POLE  
 OVERHEAD SIGN SUPPORT  
 DENOTES SIGN NUMBER  
(X) INDICATES SIGN SIZE  
E.T.R. EXISTING SIGN TO REMAIN  
L.P. LIGHT POLE



SHEET 5 OF 13

PROJECT NO:1104-12-70

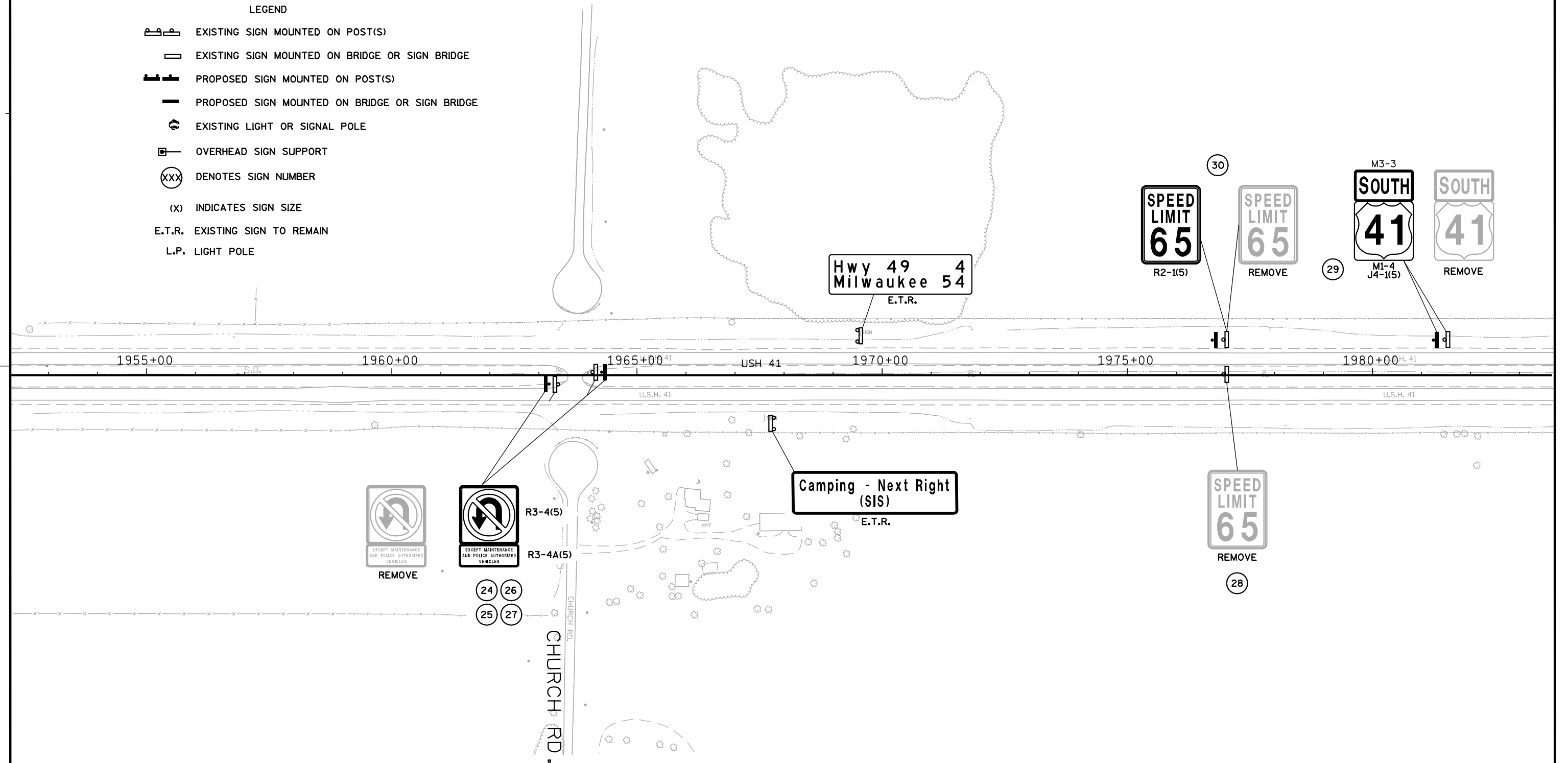
HWY: USH 41

COUNTY:FOND DU LAC

PERMANENT SIGNING

SHEET

E

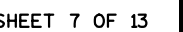


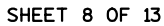
|   |  |
|---|--|
|  | EXISTING SIGN MOUNTED ON POST(S)               |
|  | EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | PROPOSED SIGN MOUNTED ON POST(S)               |
|  | PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | EXISTING LIGHT OR SIGNAL POLE                  |
|  | OVERHEAD SIGN SUPPORT                          |

    DENOTES SIGN NUMBER








E.T.R. EXISTING SIGN TO REMAIN

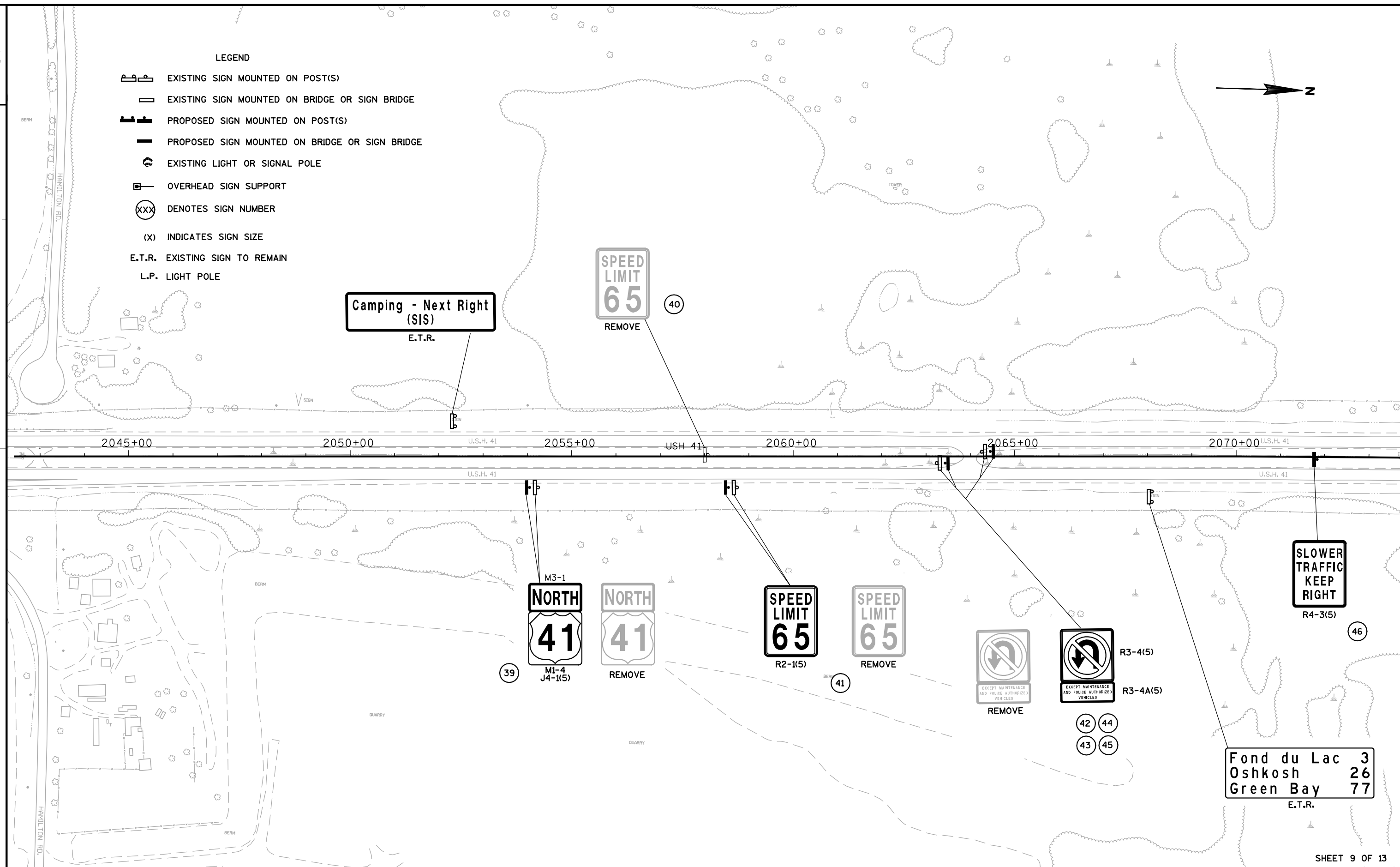
L.P. LIGHT POLE





## LEGEND








-  EXISTING SIGN MOUNTED ON POST(S)  
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 PROPOSED SIGN MOUNTED ON POST(S)  
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE  
 EXISTING LIGHT OR SIGNAL POLE  
 OVERHEAD SIGN SUPPORT  
 DENOTES SIGN NUMBER  
  
(X) INDICATES SIGN SIZE  
E.T.R. EXISTING SIGN TO REMAIN  
L.P. LIGHT POLE

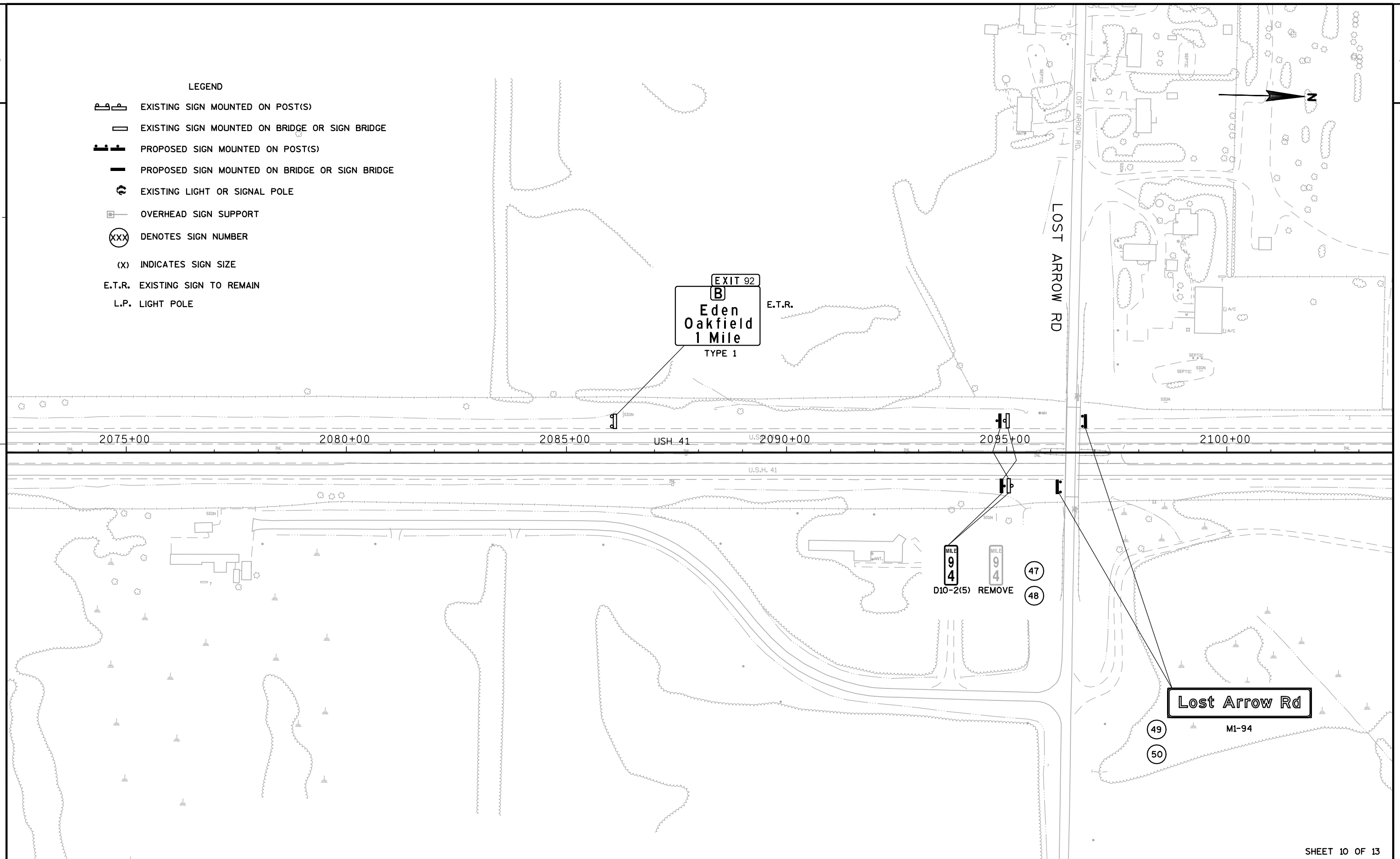


**2**

2 |

### LEGEND

- |   |  |
|---|--|
|  | EXISTING SIGN MOUNTED ON POST(S)               |
|  | EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | PROPOSED SIGN MOUNTED ON POST(S)               |
|  | PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | EXISTING LIGHT OR SIGNAL POLE                  |
|  | OVERHEAD SIGN SUPPORT                          |
|  | DENOTES SIGN NUMBER                            |
| (X)   | INDICATES SIGN SIZE                            |
| E.T.R.  | EXISTING SIGN TO REMAIN                        |
| L.P.  | LIGHT POLE                                     |



SHEET 10 OF 13

|                        |
|------------------------|
| PROJECT NO: 1104-12-70 |
|------------------------|

HWY: USH 41

COUNTY:FOND DU LAC

PERMANENT SIGNING

SHEET

3

FILE NAME : J:\projects\D2\_11041241\70\023210\_ps.dgn

PLOT DATE : 27-JUL-2005 11:17

PLOT BY : D0TR3H

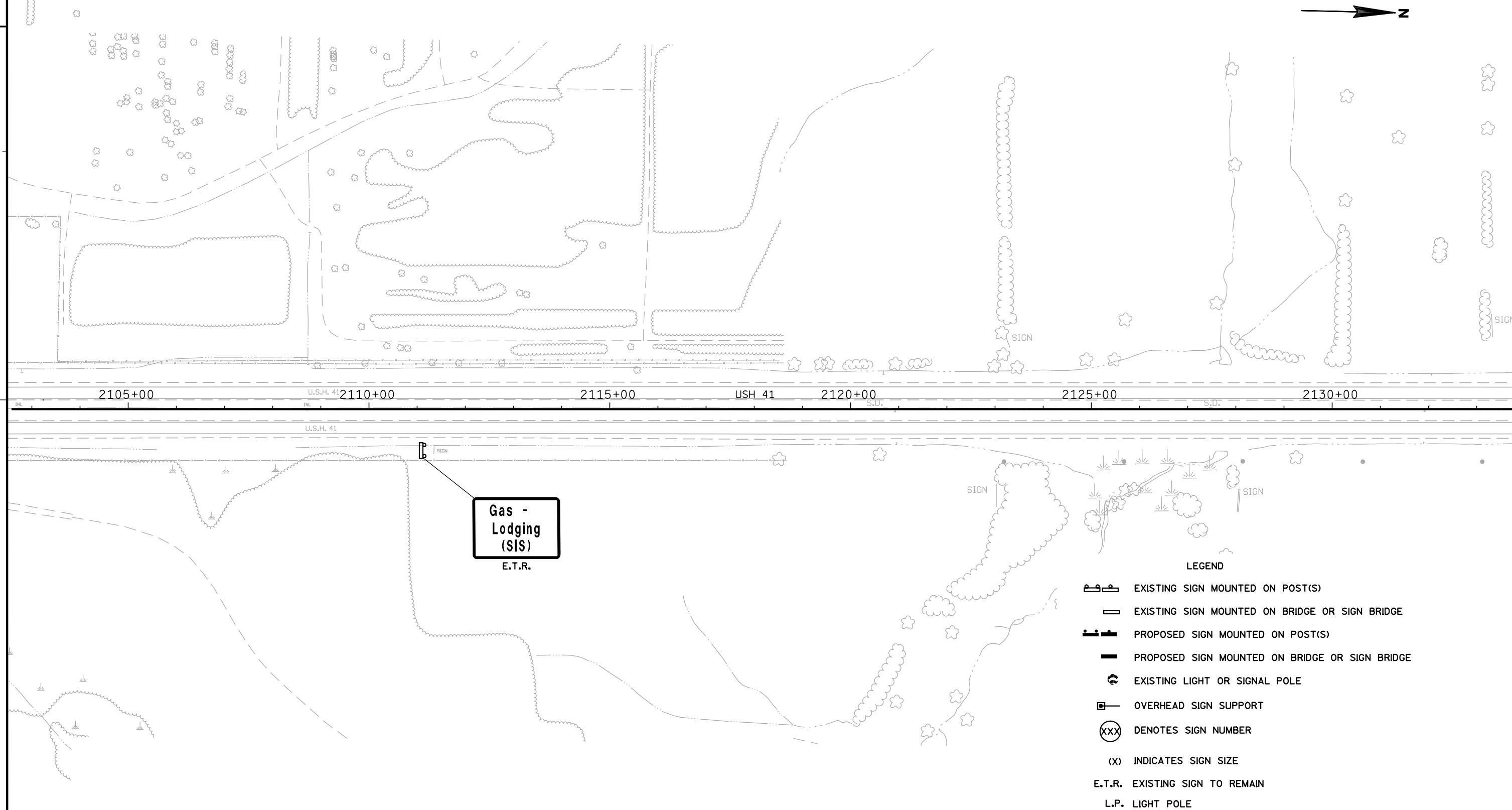
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PLOT SCALE : 200.000097:1.000000

WISDOT/CADDS SHEET 42

2

2



SHEET 11 OF 13

PROJECT NO: 1104-12-70

HWY: USH 41

COUNTY:FOND DU LAC

PERMANENT SIGNING

|       |
|-------|
| SHEET |
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|  |          |
|--|----------|
|  | <b>E</b> |
|--|----------|

FILE NAME : J:\projects\D2\_11041241\70\023211\_ps.dgn

PLOT DATE : 27-JUL-2005 11:17

PLOT BY : D0TR3H

PLOT NAME :








PLOT SCALE : 200.000013:1.000000

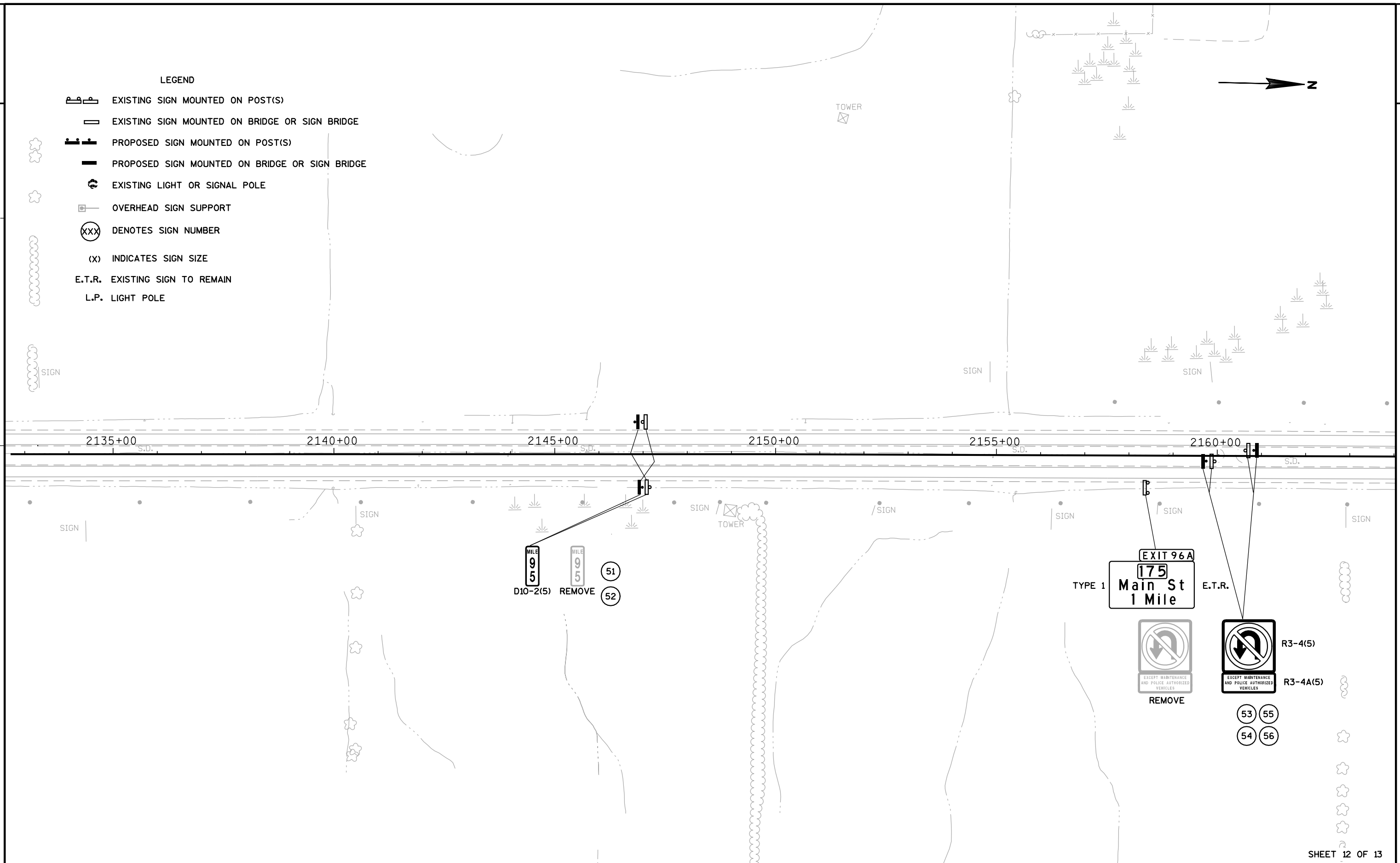
WISDOT/CADDS SHEET 42

2

2

## LEGEND

- |   |  |
|---|--|
|  | EXISTING SIGN MOUNTED ON POST(S)               |
|  | EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | PROPOSED SIGN MOUNTED ON POST(S)               |
|  | PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | EXISTING LIGHT OR SIGNAL POLE                  |
|  | OVERHEAD SIGN SUPPORT                          |
|  | DENOTES SIGN NUMBER                            |
| (X)   | INDICATES SIGN SIZE                            |
| E.T.R.  | EXISTING SIGN TO REMAIN                        |
| L.P.  | LIGHT POLE                                     |



SHEET 12 OF 13

PROJECT NO: 1104-12-70

HWY: USH 41

COUNTY:FOND DU LAC

|           |         |
|-----------|---------|
| PERMANENT | SIGNING |
|-----------|---------|

SHEET

**E**

FILE NAME : J:\projects\D2\_11041241\70\023212\_ps.dgn

PLOT DATE : 27-JUL-2005 11:17

PLOT BY : D0TR3H

PLOT NAME :

PLOT SCALE : 200.000203:1.000000

WISDOT/CADDS SHEET 42

 EXISTING SIGN MOUNTED ON POST(S)

 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE

 PROPOSED SIGN MOUNTED ON POST(S)

 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE

 EXISTING LIGHT OR SIGNAL POLE

 OVERHEAD SIGN SUPPORT

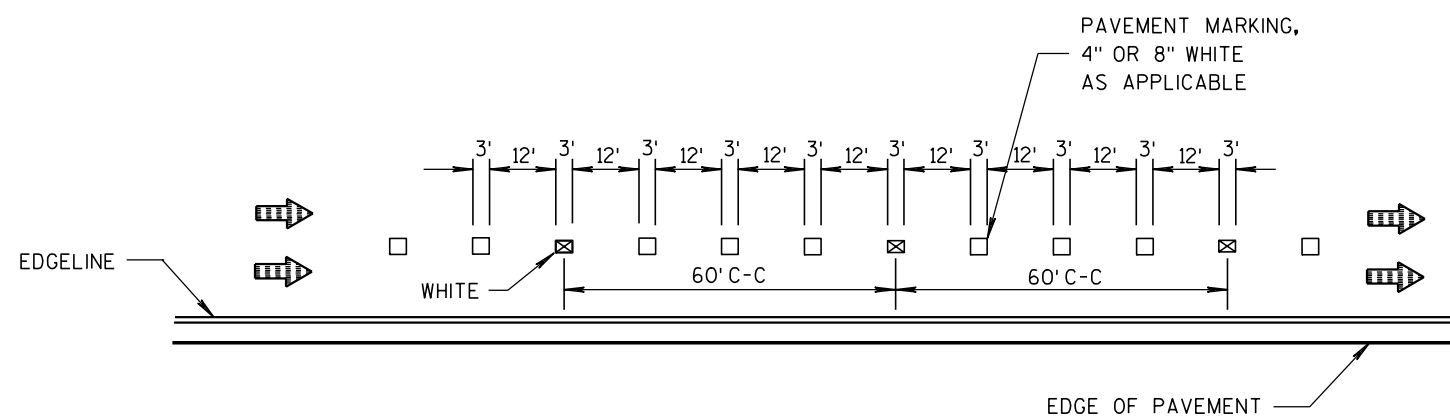
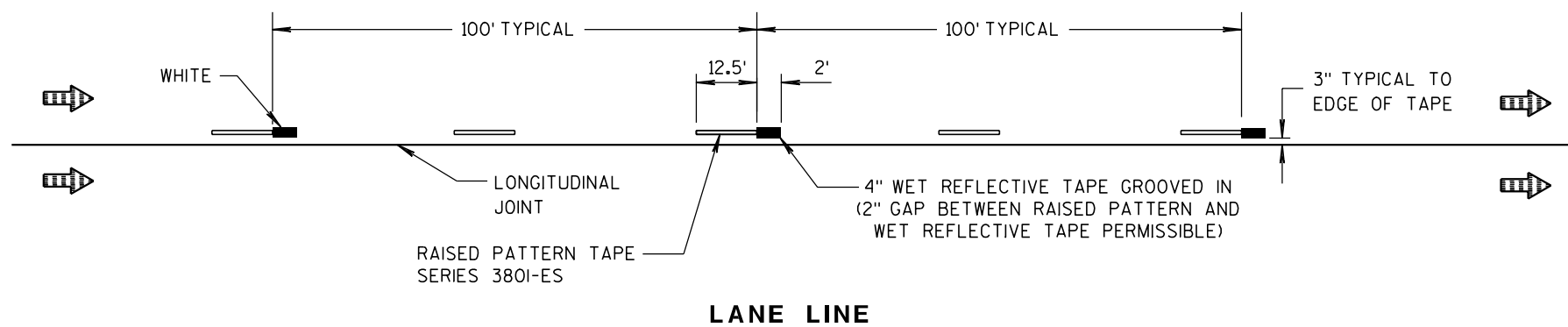
 (XXX) DENOTES SIGN NUMBER

(X) INDICATES SIGN SIZE

E.T.R. EXISTING SIGN TO REMAIN






L.P. LIGHT POLE





### TYPICAL GROOVED WET REFLECTIVE TAPE PLACEMENT

#### LEGEND

-  LANE LINE MARKINGS
-  2.0' WET REFLECTIVE TAPE
-  DIRECTION OF TRAFFIC
-  PAVEMENT ARROW
-  3.0' WET REFLECTIVE TAPE

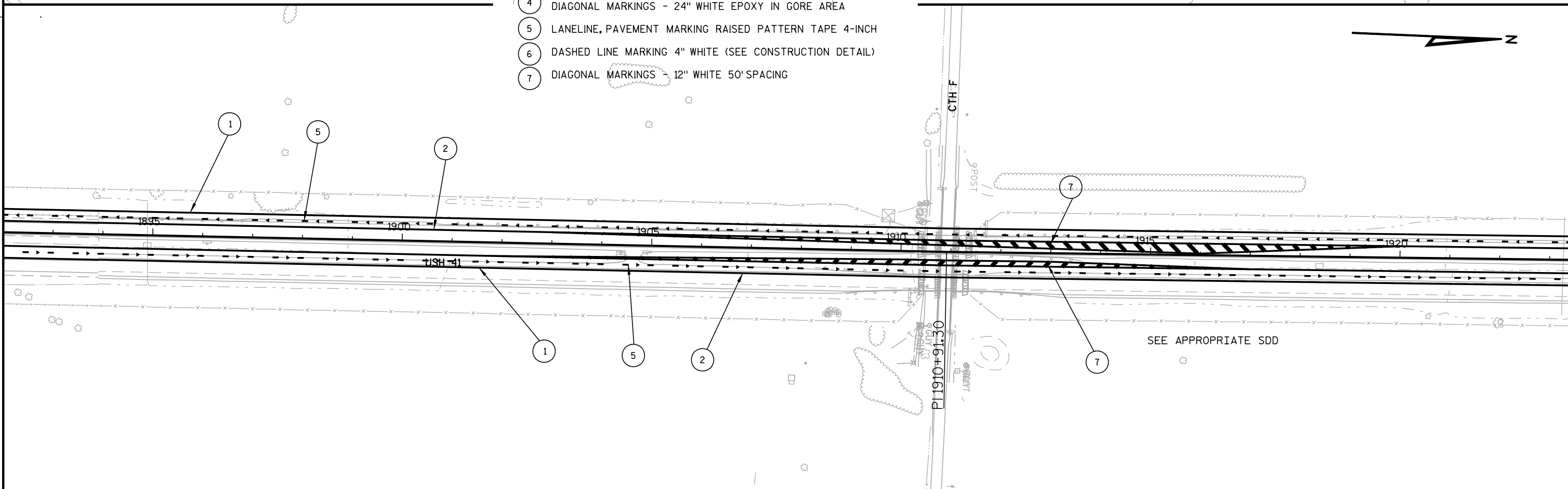
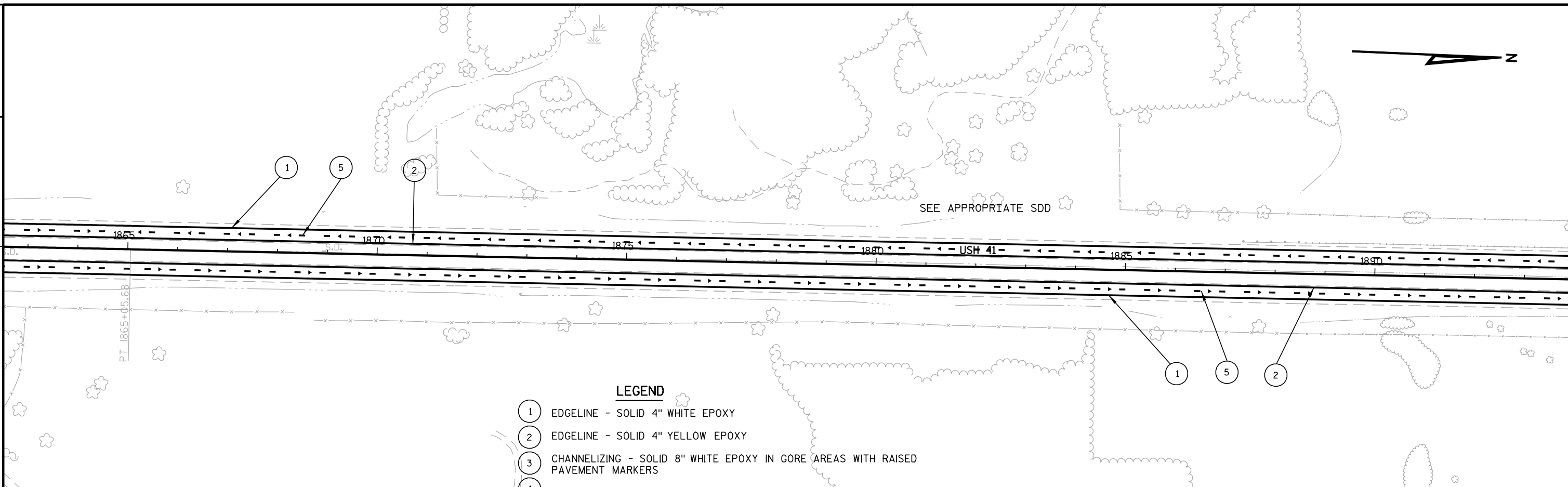
BEGIN PROJECT  
STA. 1804+56.37

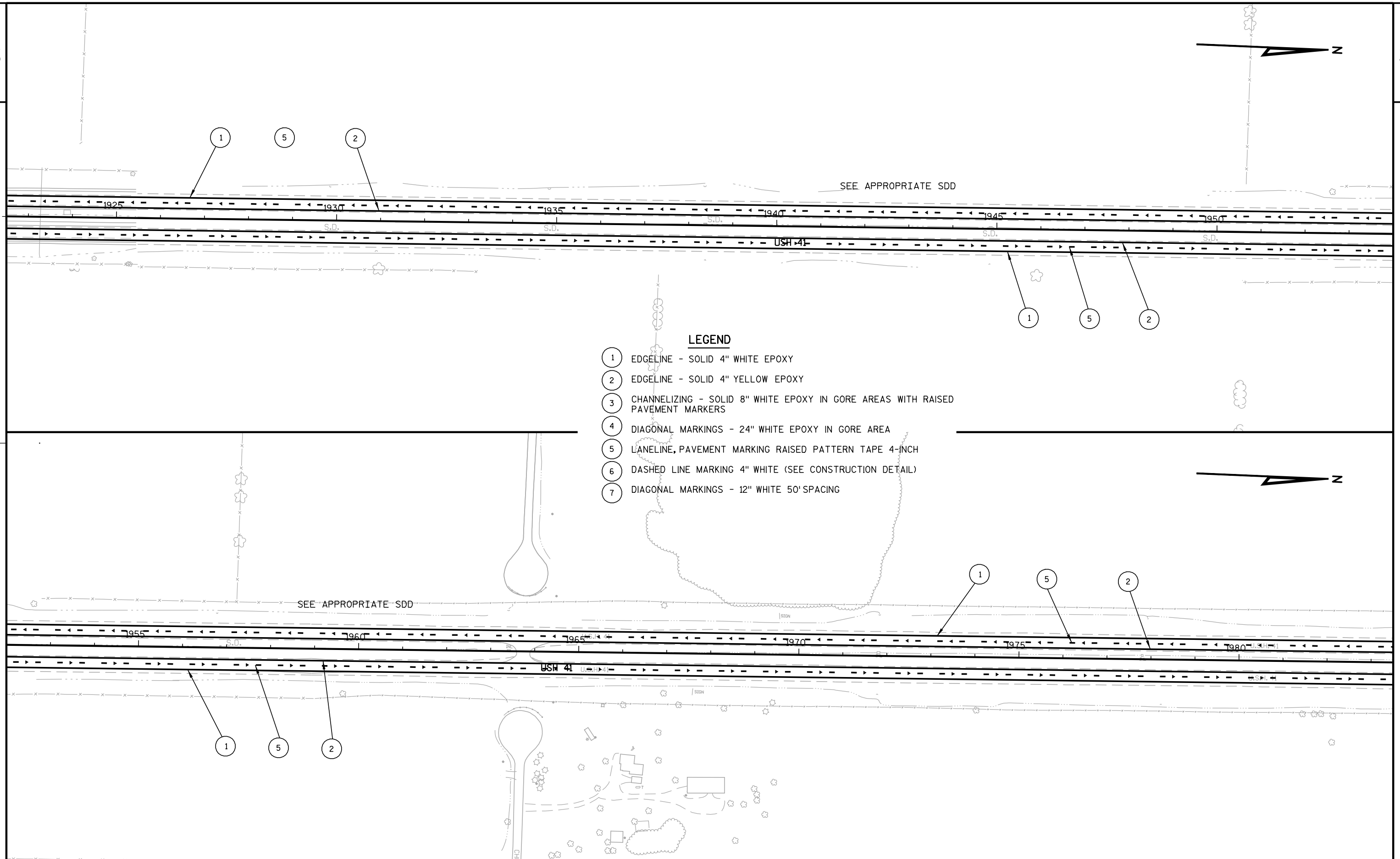
SEE APPROPRIATE SDD

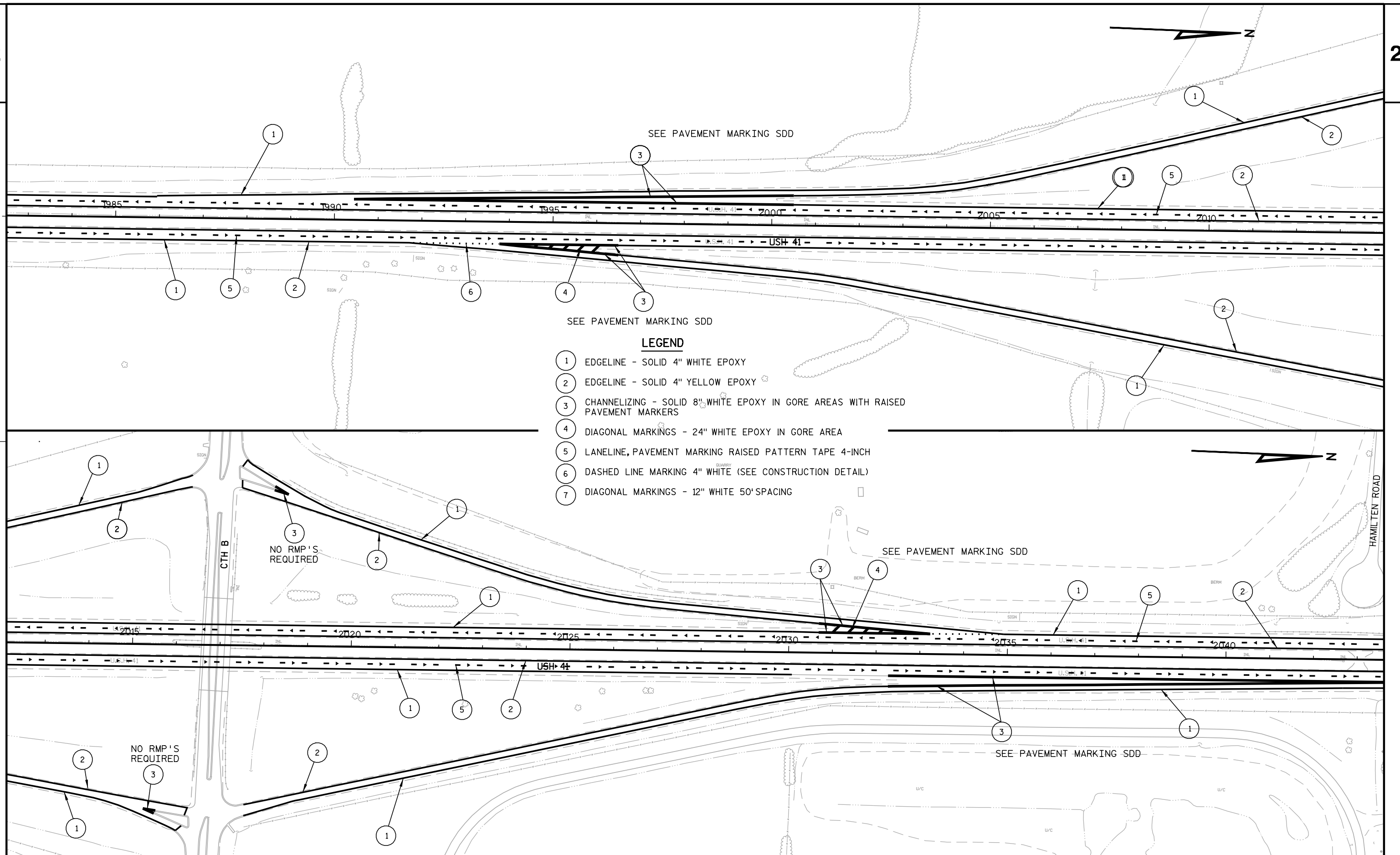
#### LEGEND

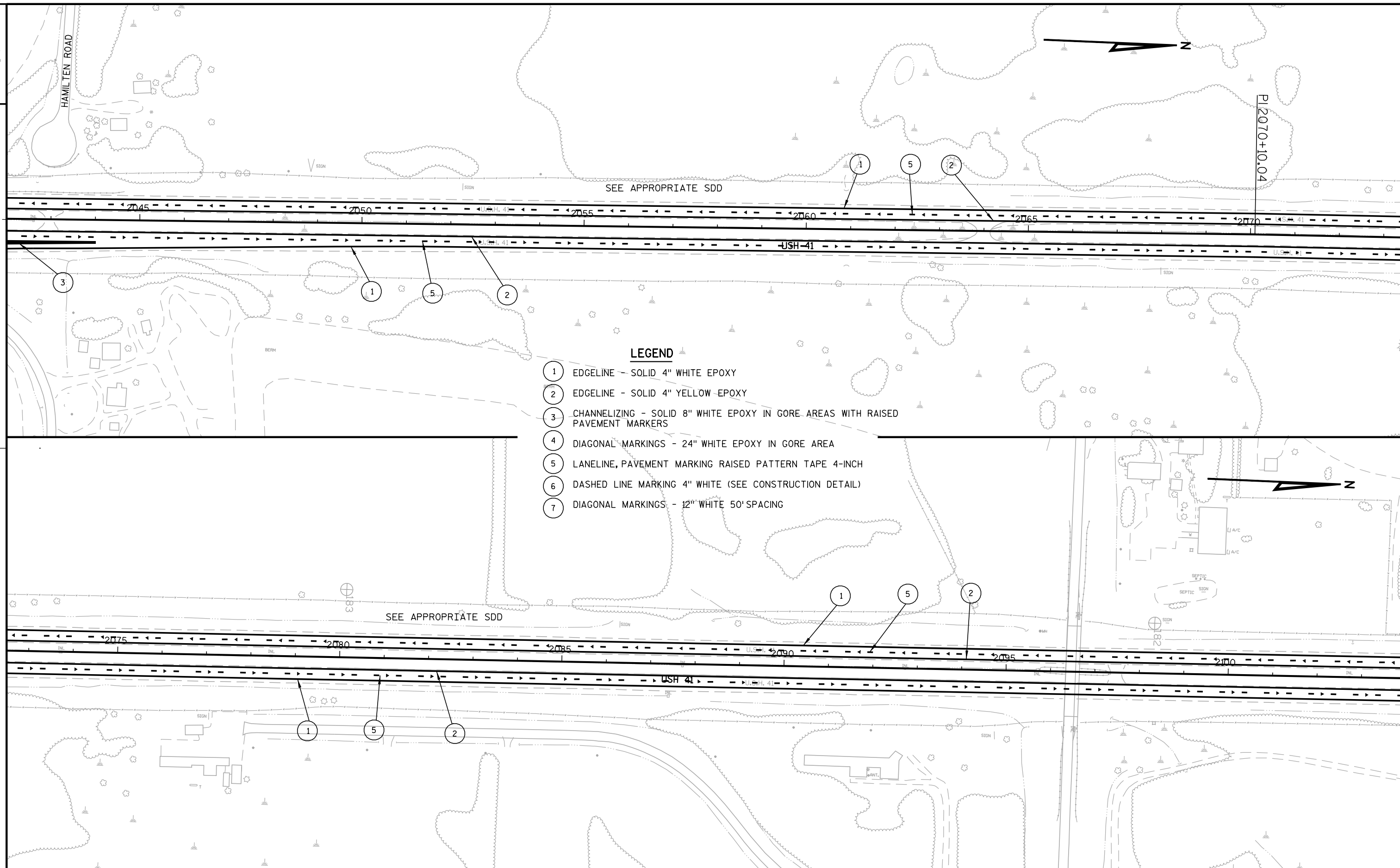
- 1 EDGELINE - SOLID 4" WHITE EPOXY
- 2 EDGELINE - SOLID 4" YELLOW EPOXY
- 3 CHANNELIZING - SOLID 8" WHITE EPOXY IN GORE AREAS WITH RAISED PAVEMENT MARKERS
- 4 DIAGONAL MARKINGS - 24" WHITE EPOXY IN GORE AREA
- 5 LANELINE, PAVEMENT MARKING RAISED PATTERN TAPE 4-INCH
- 6 DASHED LINE MARKING 4" WHITE (SEE CONSTRUCTION DETAIL)
- 7 DIAGONAL MARKINGS - 12" WHITE 50' SPACING

SEE APPROPRIATE SDD



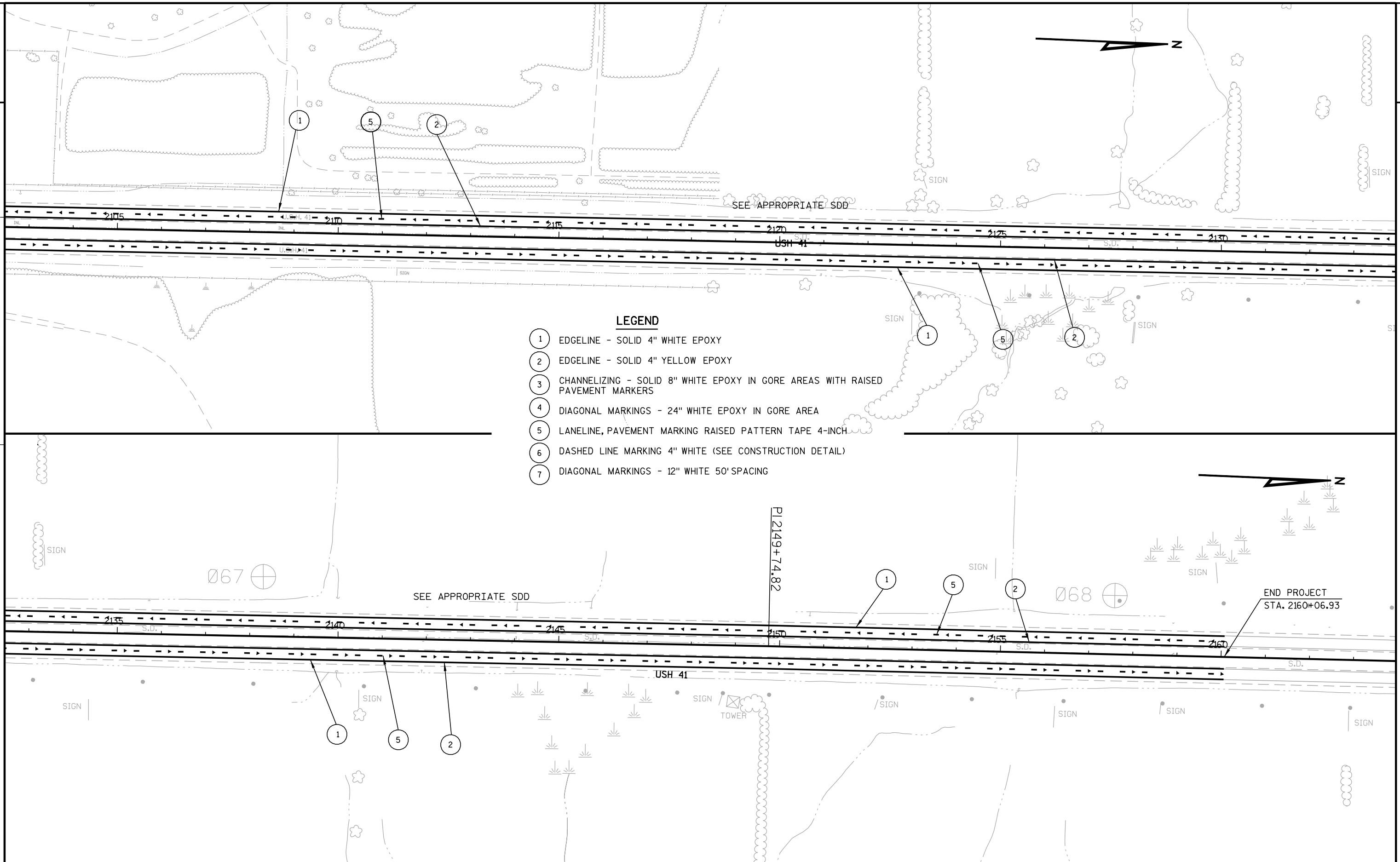






2

2



PROJECT NO:1104-12-70

HWY: USH 41

COUNTY:FOND DU LAC

PAVEMENT MARKING

SHEET

E

FILE NAME : J:\projects\02\_11041241\70\024506\_PM.dgn

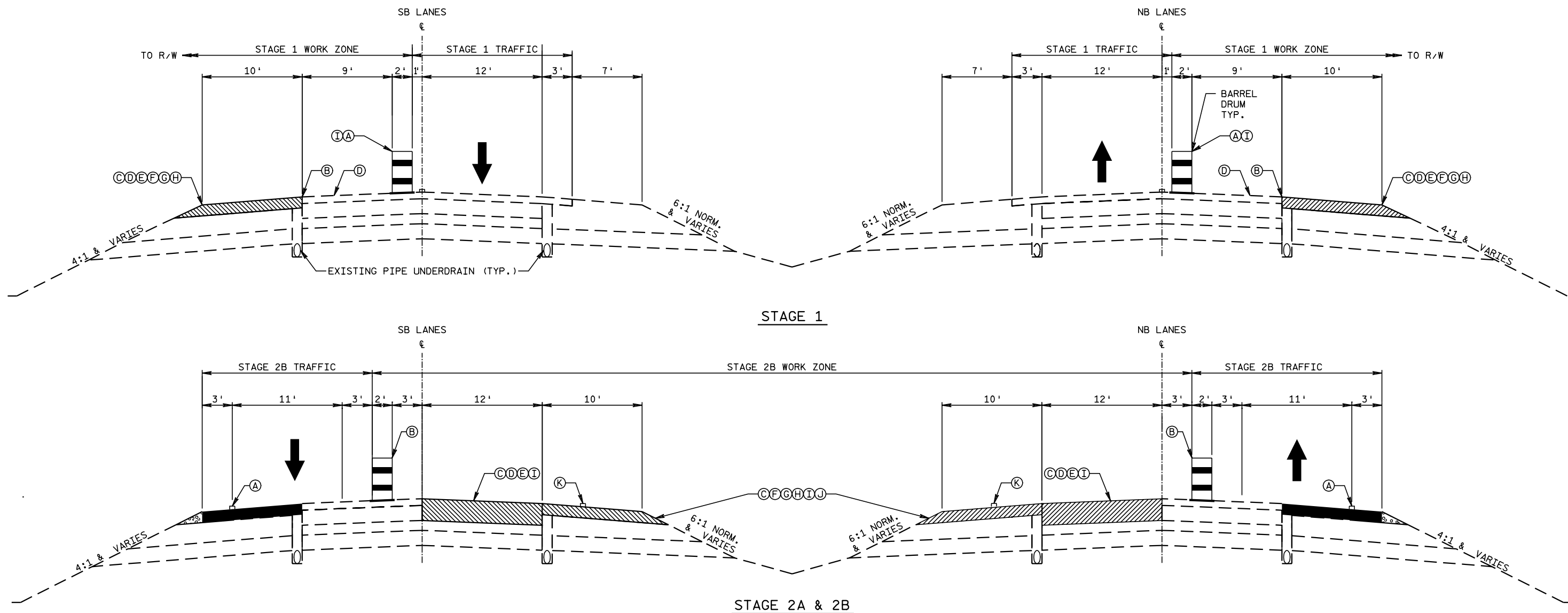
PLOT DATE : 27-JUL-2005 11:17

PLOT BY : DOTR3H

PLOT NAME :

PLOT SCALE : 200.000096:1.000000

WISDOT/CADDs SHEET 42



## GENERAL NOTES:

1. LIMIT ALL STAGED CONSTRUCTION TO A LENGTH THAT CAN BE COMPLETED WITHIN ONE WORK WEEK AND PRIOR TO OPENING ALL LANES TO WEEKEND TRAFFIC.
2. STAGING IS SHOWN FOR RUBBLIZED FINISHED SECTION. BASE PATCH FINISHED SECTION SIMILAR
3. STAGE 1 CONSTRUCTION MUST BE PERFORMED PRIOR TO STAGE 2 AND 3 CONSTRUCTION.
4. STAGE 1 CONSTRUCTION SHALL NOT BE PERFORMED PRIOR TO STAGE 2 CONSTRUCTION.
5. STAGE 2 AND 3 MAINLINE CONSTRUCTION SHALL TERMINATE AT THE SAME STATION PRIOR TO OPENING ALL LANES TO WEEKEND TRAFFIC.
6. STAGE 1, 2 AND 3 CONSTRUCTION MUST BE COMPLETE TO A DISTANCE THAT DOES NOT HINDER STAGE 4 CONSTRUCTION. START STAGE 4 CONSTRUCTION AS APPROVED BY THE ENGINEER.
7. STAGE 4A, 4B AND 4C CONSTRUCTION SHALL TERMINATE AT THE SAME STATION PRIOR TO OPENING ALL LANES TO WEEKEND TRAFFIC.
8. RAMP WORK IS NOT SHOWN FOR CLARITY. RAMP WORK MAY BE PERFORMED DURING MAINLINE CONSTRUCTION AS APPROVED AND DIRECTED BY THE ENGINEER.
9. STAGE 1 AND 2 CULVERT WORK AND RELATED BASE PATCHING NOT SHOWN FOR CLARITY. SEE CONSTRUCTION DETAILS.
10. TRAFFIC STAGING IS SHOWN FOR NORMAL CROWN SECTION.

## STAGE 1:

- A. SET UP STAGE 1 TRAFFIC CONTROL & TEMPORARY SIGNING, PLACE TEMPORARY PAVEMENT MARKINGS, SEE TRAFFIC STAGING PLAN
- B. SAWING EXISTING PAVEMENT
- C. REMOVE OUTSIDE SHOULDER (PAID FOR AS ASPHALTIC SURFACE)
- D. PERFORM CULVERT WORK AND RELATED BASE PATCHING\*
- E. PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS
- F. PERFORM TEST ROLLING\*\*
- G. PLACE SMA PAVEMENT (LOWER LAYER) 5" MINIMUM
- H. PLACE BASE AGGREGATE DENSE  $\frac{3}{4}$  INCH
- I. REMOVE TRAFFIC CONTROL, COVER CONFLICTING SIGNING AND REMOVE CONFLICTING PAVEMENT MARKINGS FOR WEEKEND TRAFFIC

## STAGE 2A:

- A. PLACE TEMPORARY PAVEMENT MARKINGS AND REMOVE CONFLICTING PAVEMENT MARKINGS FOR STAGE 2B TRAFFIC

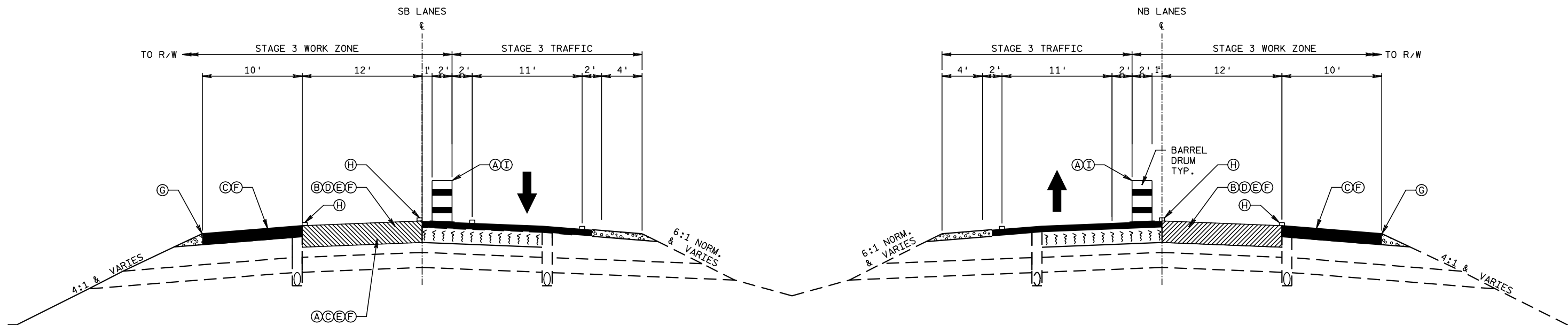
## STAGE 2B:

- B. SET UP STAGE 2 TRAFFIC CONTROL & TEMPORARY SIGNING
- C. PERFORM CULVERT WORK AND RELATED BASE PATCHING\*
- D. PERFORM REMOVING ASPHALTIC SURFACE MILLING FULL DEPTH
- E. RUBBLIZE CONCRETE PAVEMENT
- F. REMOVE INSIDE SHOULDER (PAID FOR AS ASPHALTIC SURFACE MILLING)
- G. PERFORM TEST ROLLING\*\* ON INSIDE SHOULDER
- H. PLACE BASE AGGREGATE DENSE  $1\frac{3}{4}$  INCH TO REQUIRED LEVEL FOR PLACEMENT OF INSIDE ASPHALTIC SHOULDER
- I. PLACE SMA PAVEMENT ON INSIDE SHOULDER 5" MINIMUM AND LANE SIMULTANEOUSLY (LOWER LAYERS)
- J. PLACE BASE AGGREGATE DENSE  $1\frac{1}{4}$  INCH AND  $\frac{3}{4}$  INCH ACCORDINGLY
- K. PLACE TEMPORARY PAVEMENT MARKINGS FOR STAGE 3 TRAFFIC

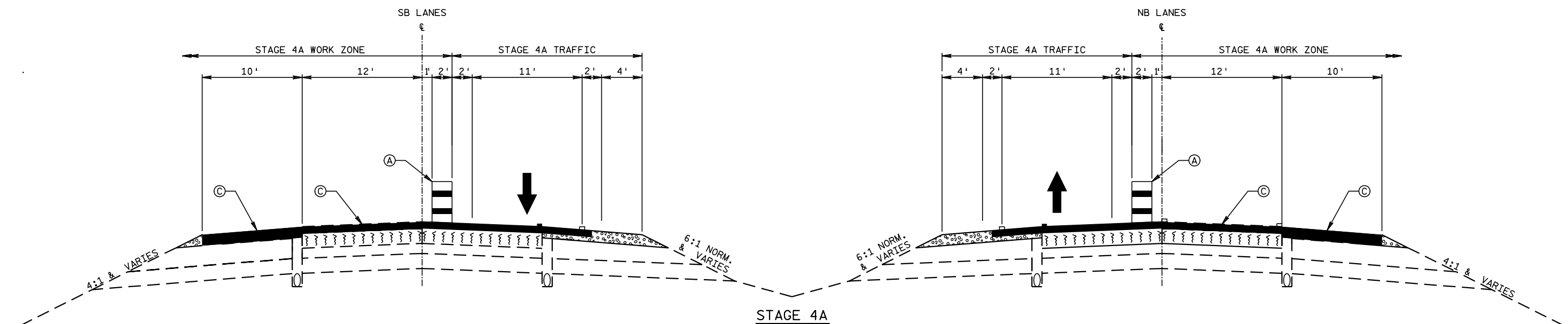
\* CULVERT WORK AND BASE PATCHING NOT SHOWN FOR CLARITY, SEE CONSTRUCTION DETAILS.

\*\* CONTRACTOR TO PERFORM EXCAVATION BELOW SUBGRADE WITH BORROW EXCAVATION WITHIN LIMITS OF OUTSIDE SHOULDER IF DIRECTED BY ENGINEER.

\*\*\* CONCRETE BASE PATCHING (SHES) AND ASPHALTIC PATCHING IN LIEU OF RUBBLIZATION AS SHOWN ON THE PLANS.



STAGE 3



STAGE 4A

**STAGE 3:**

- A. SWITCH TRAFFIC TO STAGE 3 TRAFFIC CONTROL, SEE TRAFFIC STAGING PLAN
- B. PERFORM REMOVING ASPHALTIC SURFACE MILLING FULL DEPTH
- C. PERFORM REMOVING ASPHALTIC SURFACE MILLING ON OUTSIDE SHOULDER (IF REQUIRED)
- D. RUBBLIZE CONCRETE PAVEMENT\*\*\*
- E. PLACE WARRANTED SMA PAVEMENT ON OUTSIDE LANE (LOWER LAYER)
- F. PLACE WARRANTED SMA PAVEMENT ON OUTSIDE LANE AND SHOULDER SIMULTANEOUSLY (LOWER LAYER)
- G. PLACE BASE AGGREGATE DENSE  $\frac{3}{4}$  INCH ACCORDINGLY
- H. PLACE TEMPORARY PAVEMENT MARKINGS FOR STAGE 3 TRAFFIC
- I. REMOVE TRAFFIC CONTROL AND COVER CONFLICTING SIGNING

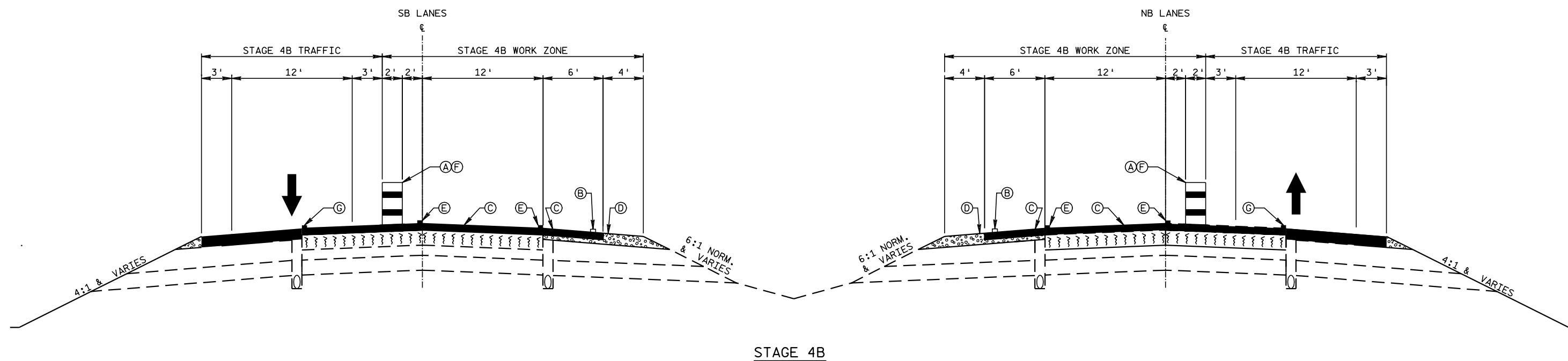
**STAGE 4A:**

- A. SET UP STAGE 4A TRAFFIC CONTROL, SEE TRAFFIC STAGING PLAN
- B. REMOVE TEMPORARY PAVEMENT MARKINGS
- C. PLACE WARRANTED SMA PAVEMENT ON OUTSIDE LANE AND SHOULDER SIMULTANEOUSLY (UPPER LAYER)

\* CULVERT WORK AND BASE PATCHING NOT SHOWN FOR CLARITY, SEE CONSTRUCTION DETAILS.

\*\* CONTRACTOR TO PERFORM EXCAVATION BELOW SUBGRADE WITH BORROW EXCAVATION WITHIN LIMITS OF OUTSIDE SHOULDER IF DIRECTED BY ENGINEER.

\*\*\* CONCRETE BASE PATCHING (SHES) AND ASPHALTIC BASE PATCHING IN LIEU OF RUBBLIZATION AS SHOWN ON THE PLANS.

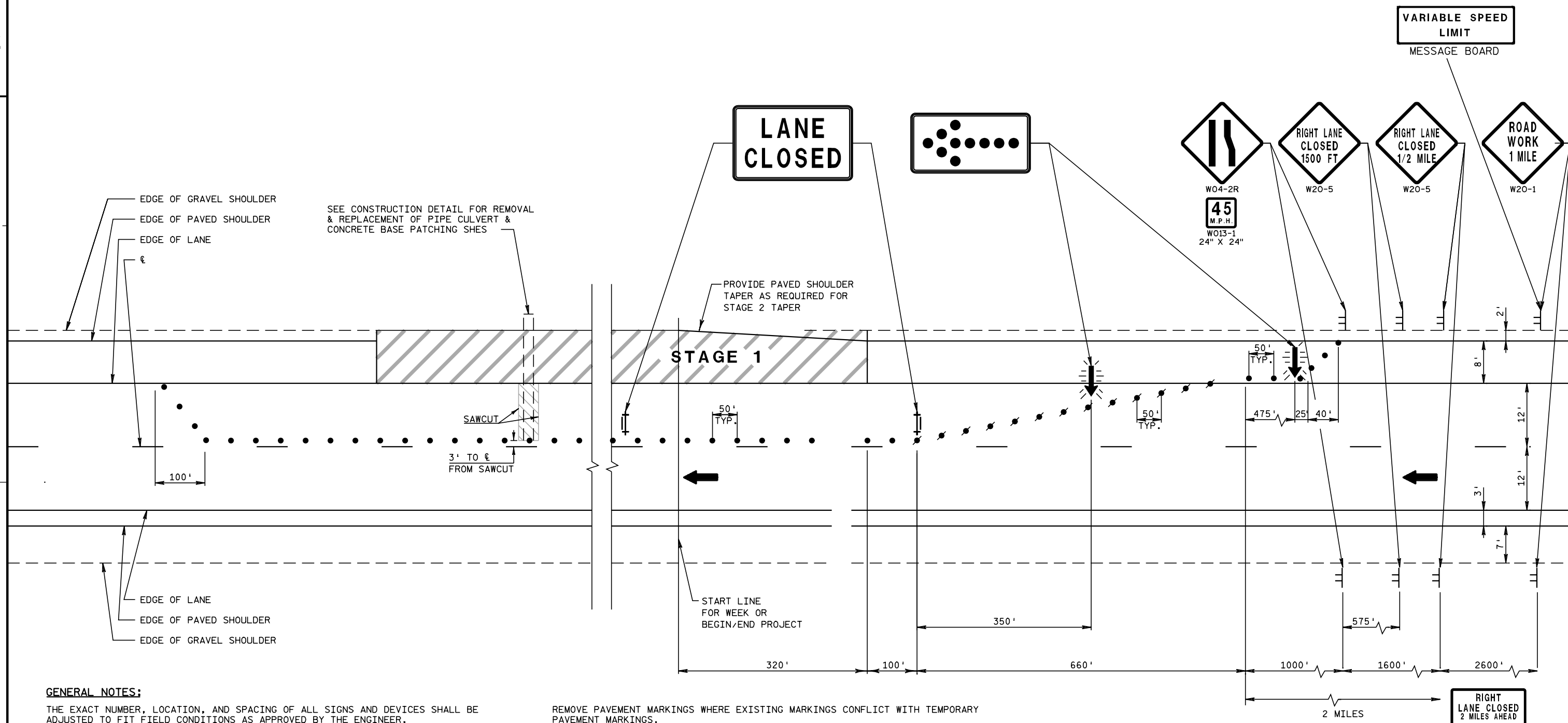


**STAGE 4B:**

- A. SWITCH TRAFFIC TO STAGE 4B TRAFFIC CONTROL, SEE TRAFFIC STAGING PLAN
- B. REMOVE TEMPORARY PAVEMENT MARKINGS
- C. PLACE SMA PAVEMENT ON INSIDE LANE AND SHOULDER SIMULTANEOUSLY (UPPER LAYER)
- D. SHAPING SHOULDERS
- E. PLACE PERMANENT PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS
- F. REMOVE TRAFFIC CONTROL AND REMOVE TEMPORARY SIGNING

**STAGE 4C:**

- G. PLACE PERMANENT PAVEMENT MARKINGS FOR OUTSIDE LANE

**GENERAL NOTES:**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS WHERE EXISTING MARKINGS CONFLICT WITH TEMPORARY PAVEMENT MARKINGS.

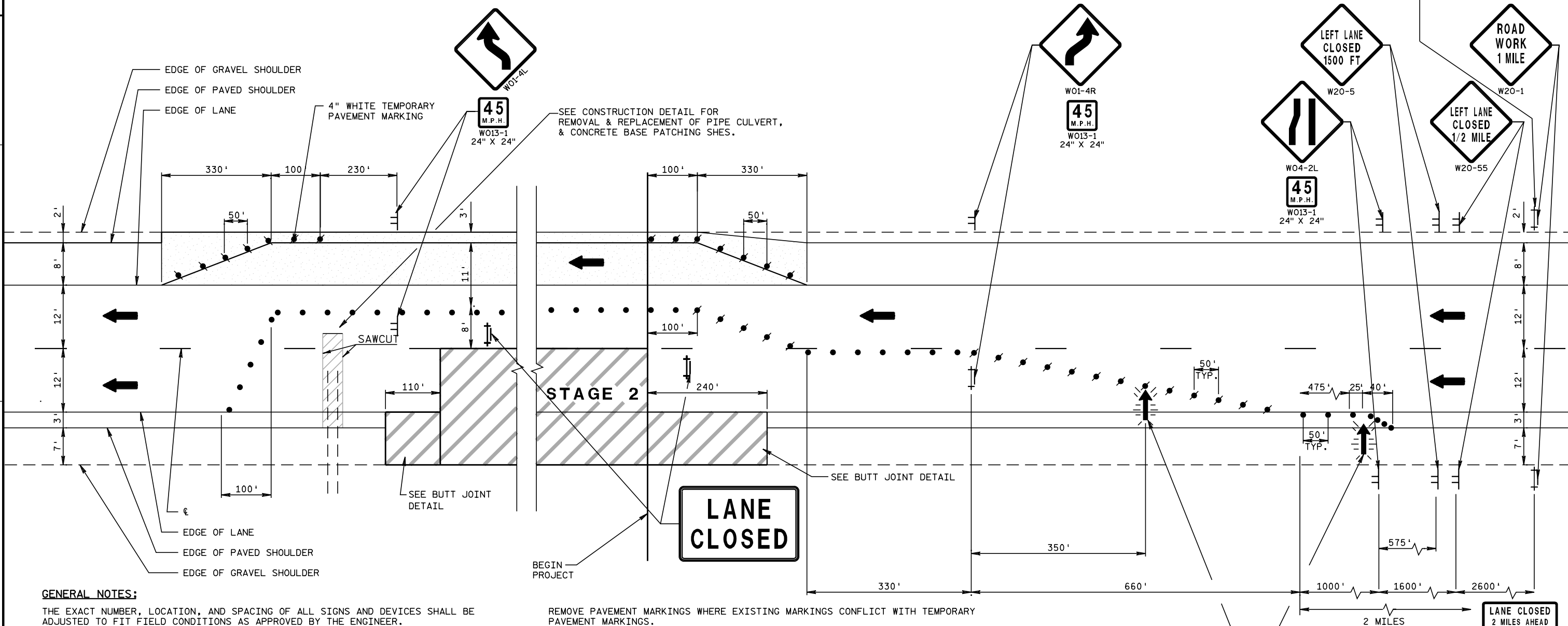
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

IF LANE CLOSURE IS MORE THAN 1 MILE, PLACE A TYPE III BARRICADE WITH 2 TYPE A LIGHTS APPROXIMATELY EVERY 1/4 MILE ACROSS THE CLOSED LANE TO HELP ENFORCE THE DRUM LINE.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP, THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE, THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

**LEGEND:**

- FLASHING ARROW BOARD
  - MOVEABLE SIGN
  - TYPE III BARRICADE WITH SIGN
  - DRUM
  - DRUM WITH TYPE "C" LIGHT
  - EDGE OF DRIVING LANE
  - DIRECTION OF TRAFFIC
- PORTABLE CHANGEABLE MESSAGE SIGN

**GENERAL NOTES:**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

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FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

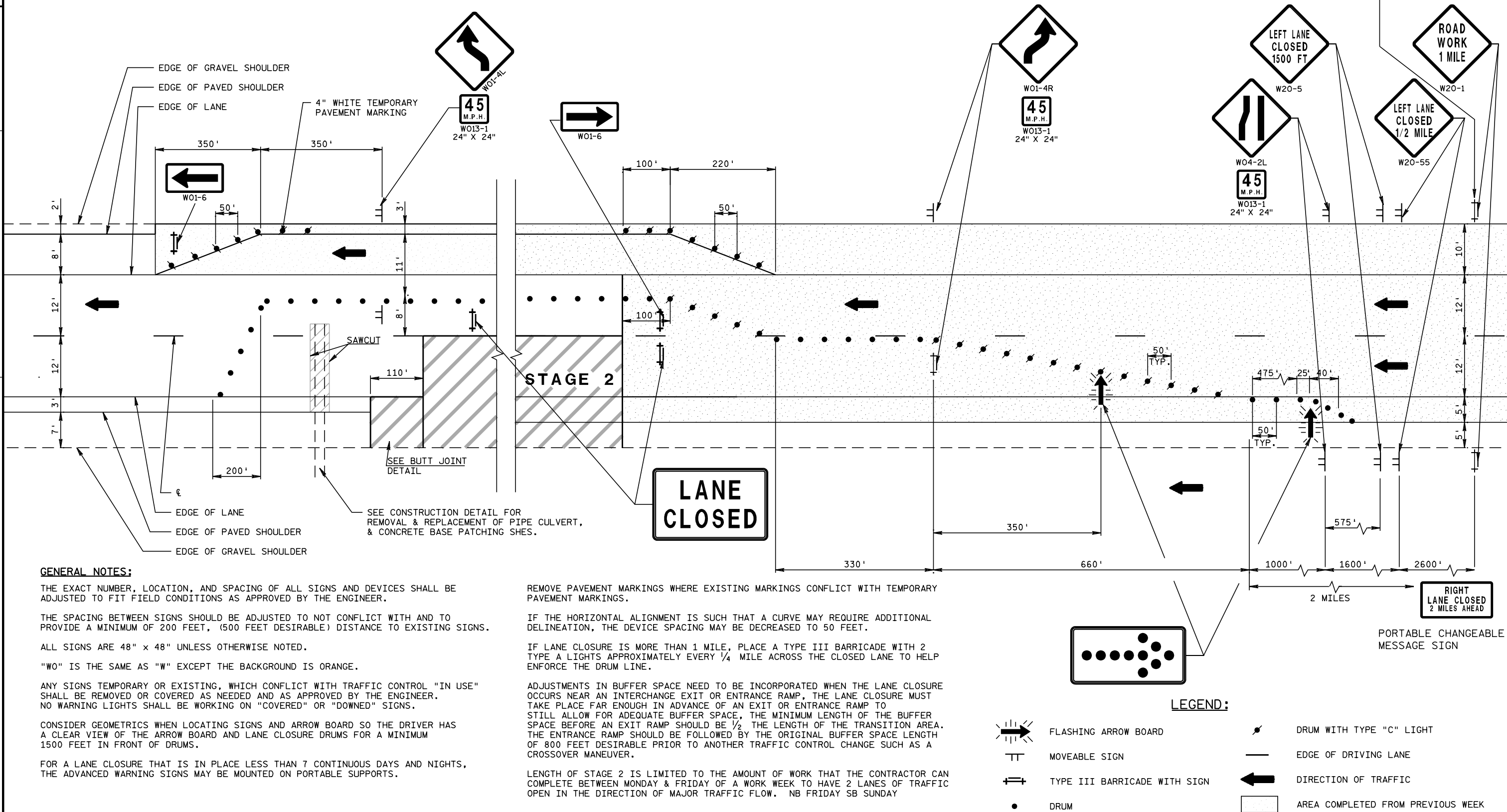
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LENGTH OF STAGE 2 IS LIMITED TO THE AMOUNT OF WORK THAT THE CONTRACTOR CAN COMPLETE BETWEEN MONDAY & FRIDAY OF A WORK WEEK TO HAVE 2 LANES OF TRAFFIC OPEN IN THE DIRECTION OF MAJOR TRAFFIC FLOW. NB FRIDAY SB SUNDAY



**GENERAL NOTES:**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

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CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS WHERE EXISTING MARKINGS CONFLICT WITH TEMPORARY PAVEMENT MARKINGS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

IF LANE CLOSURE IS MORE THAN 1 MILE, PLACE A TYPE III BARRICADE WITH 2 TYPE A LIGHTS APPROXIMATELY EVERY 1/4 MILE ACROSS THE CLOSED LANE TO HELP ENFORCE THE DRUM LINE.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP, THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE, THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

**LEGEND:**

FLASHING ARROW BOARD



MOVEABLE SIGN



TYPE III BARRICADE WITH SIGN



DIRECTION OF TRAFFIC



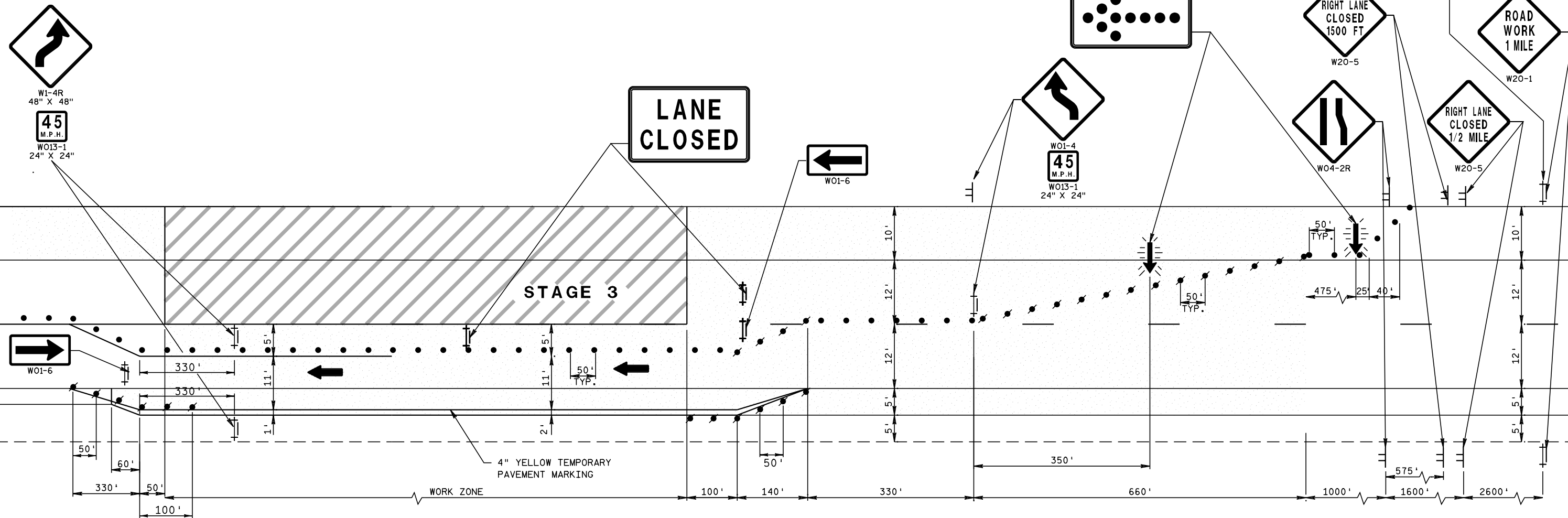
DRUM

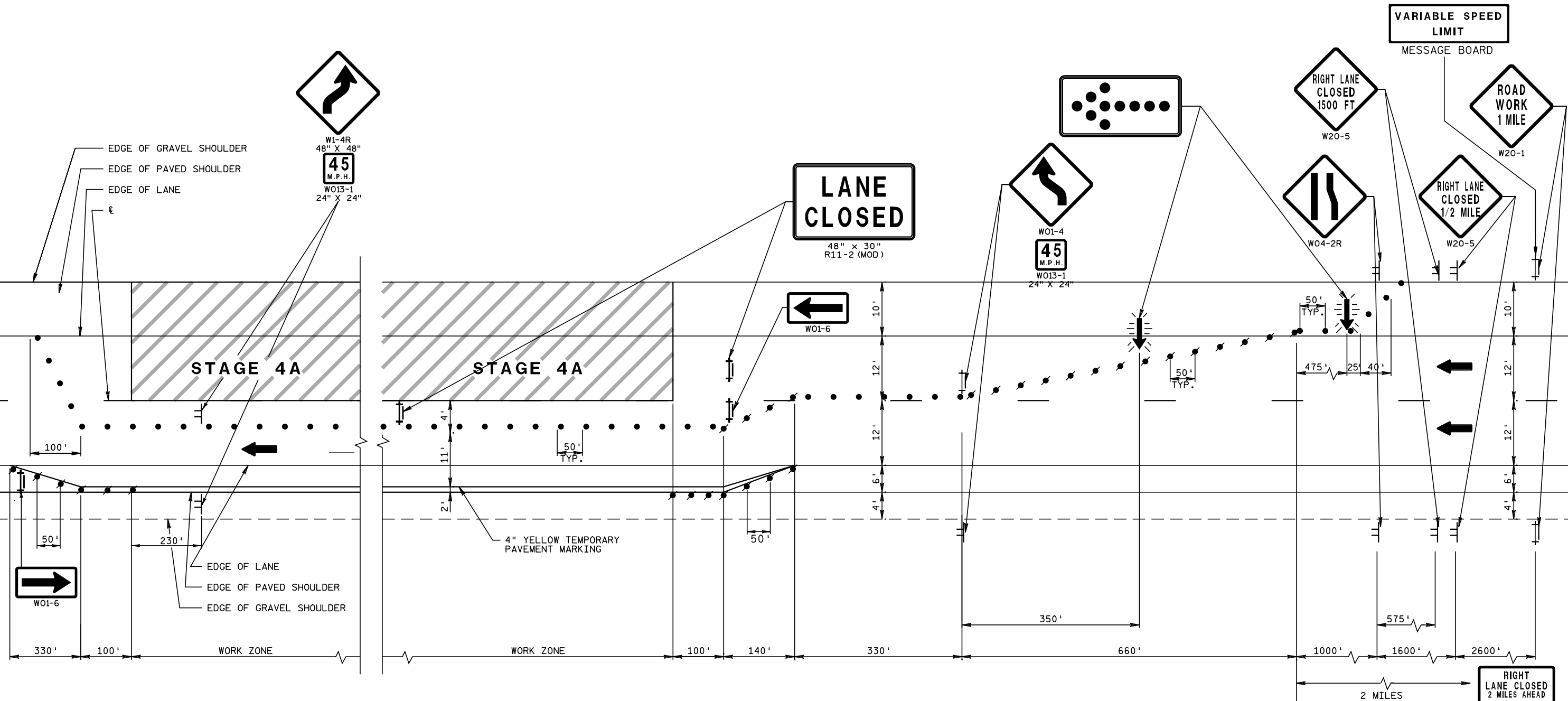


DRUM WITH TYPE "C" LIGHT



EDGE OF DRIVING LANE



**GENERAL NOTES:**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

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CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS WHERE EXISTING MARKINGS CONFLICT WITH TEMPORARY PAVEMENT MARKINGS.

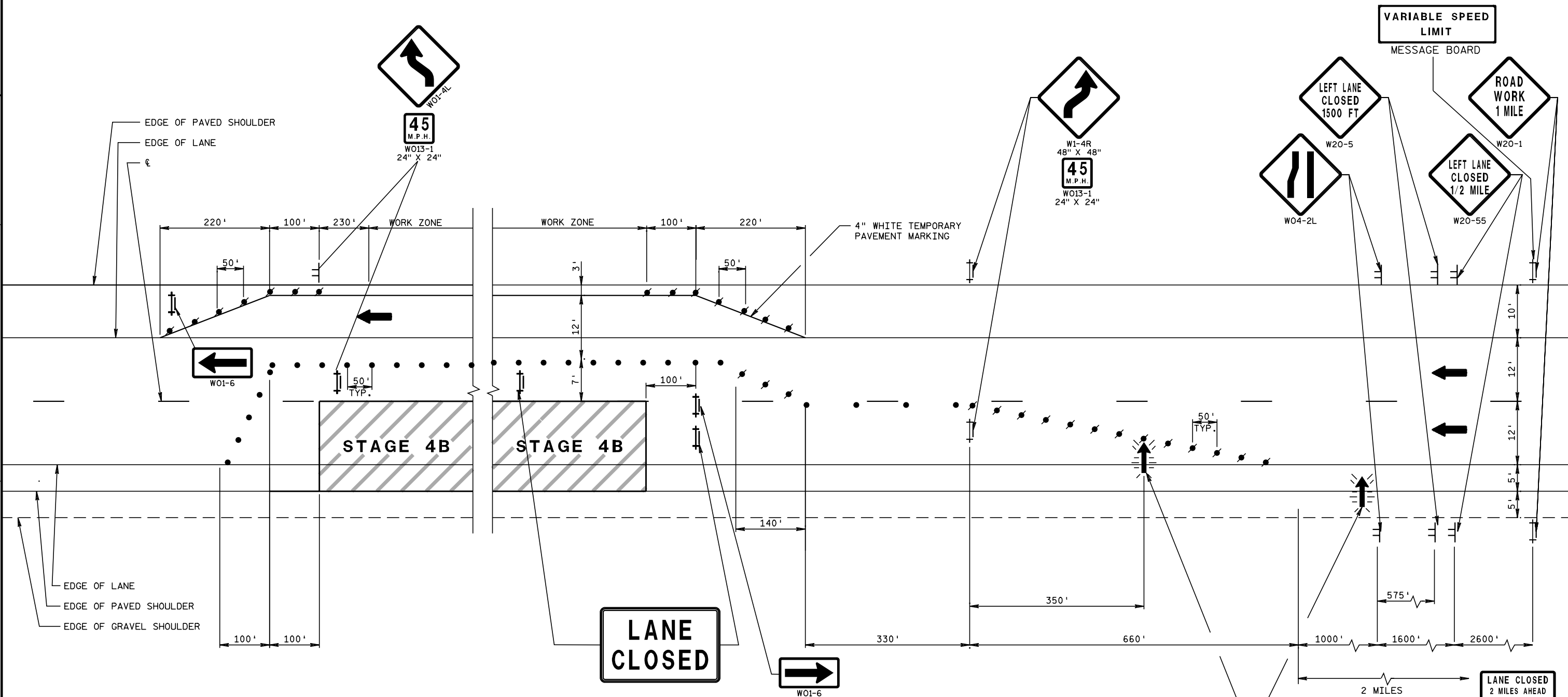
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ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP, THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE, THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

**LEGEND:**

- FLASHING ARROW BOARD
- MOVEABLE SIGN
- TYPE III BARRICADE WITH SIGN
- DRUM
- DRUM WITH TYPE "C" LIGHT
- EDGE OF DRIVING LANE
- DIRECTION OF TRAFFIC

**GENERAL NOTES:**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

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ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS WHERE EXISTING MARKINGS CONFLICT WITH TEMPORARY PAVEMENT MARKINGS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

IF LANE CLOSURE IS MORE THAN 1 MILE, PLACE A TYPE III BARRICADE WITH 2 TYPE A LIGHTS APPROXIMATELY EVERY 1/4 MILE ACROSS THE CLOSED LANE TO HELP ENFORCE THE DRUM LINE.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP, THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE, THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

**LEGEND:**

- FLASHING ARROW BOARD
- MOVEABLE SIGN
- TYPE III BARRICADE WITH SIGN
- DRUM
- DRUM WITH TYPE "C" LIGHT
- EDGE OF DRIVING LANE
- DIRECTION OF TRAFFIC

## GENERAL NOTES:

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

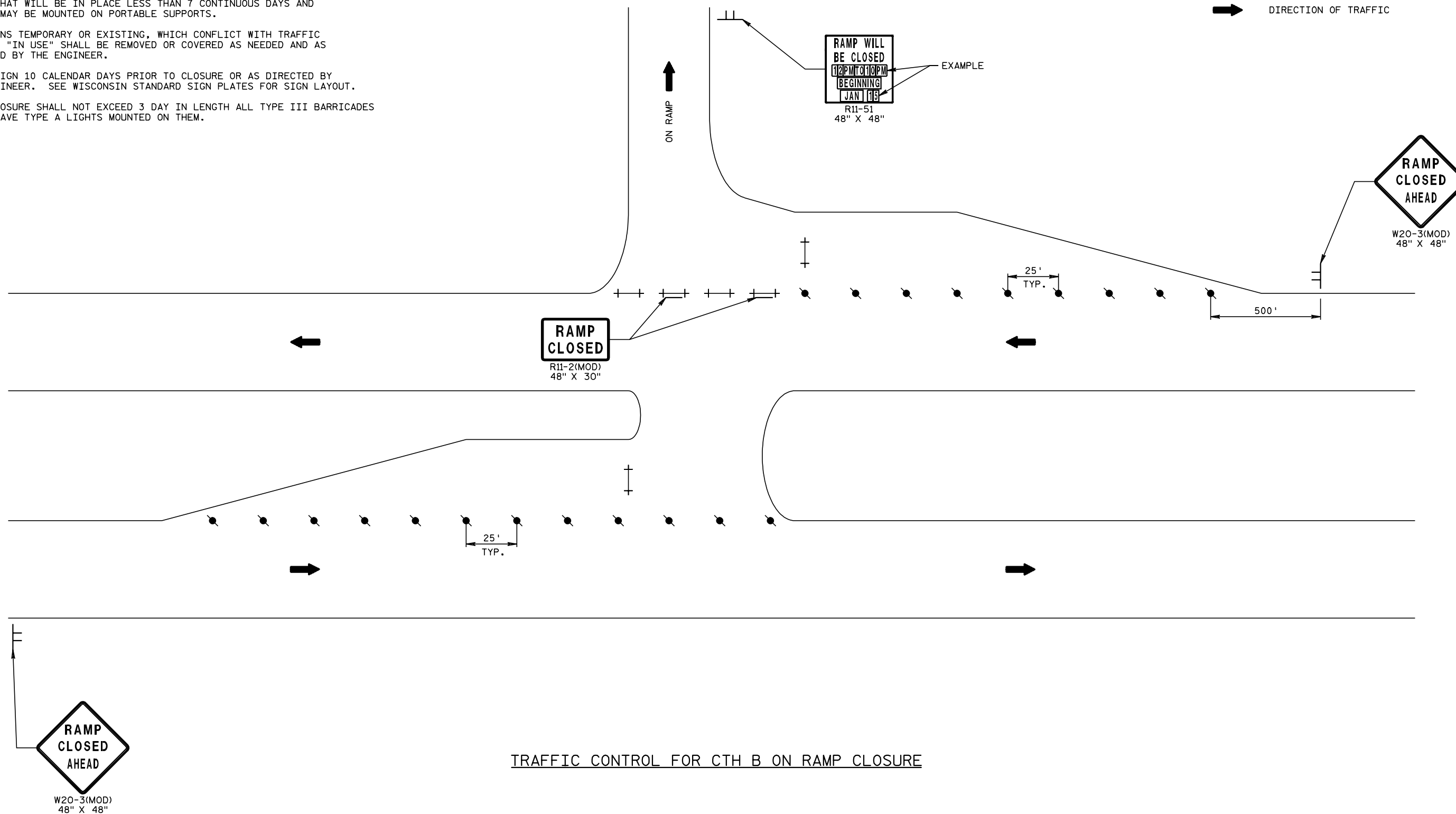
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

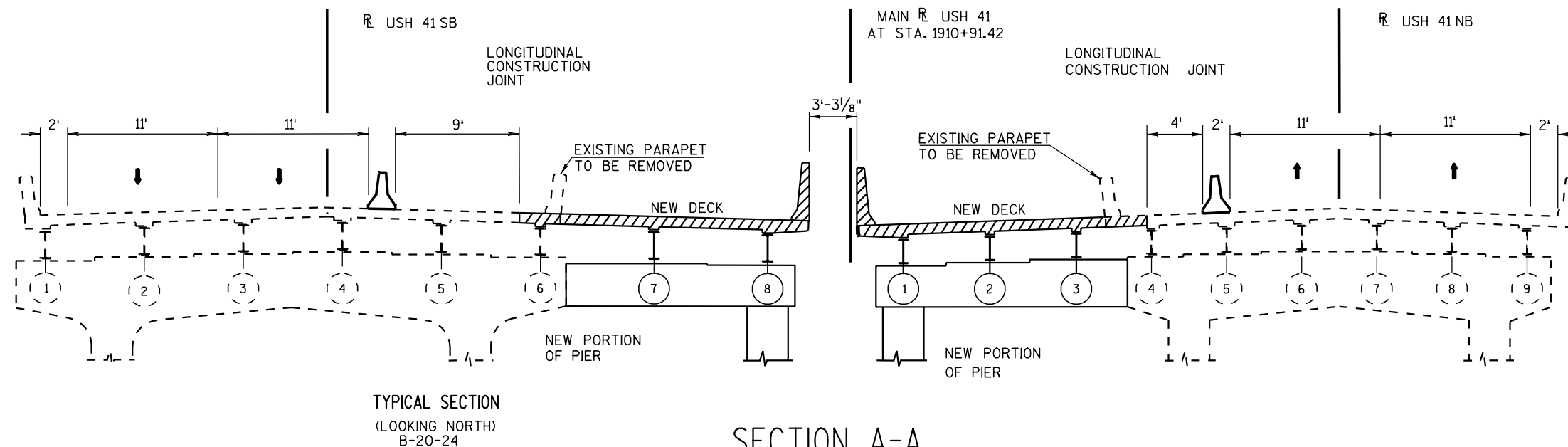
\* PLACE SIGN 10 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.

RAMP CLOSURE SHALL NOT EXCEED 3 DAY IN LENGTH ALL TYPE III BARRICADES SHALL HAVE TYPE A LIGHTS MOUNTED ON THEM.

## LEGEND:

- TYPE III BARRICADE (8' EQUIVALENT) WITH/WITHOUT SIGN
- DRUM WITH TYPE C LIGHT
- POST MOUNTED SIGN
- DIRECTION OF TRAFFIC





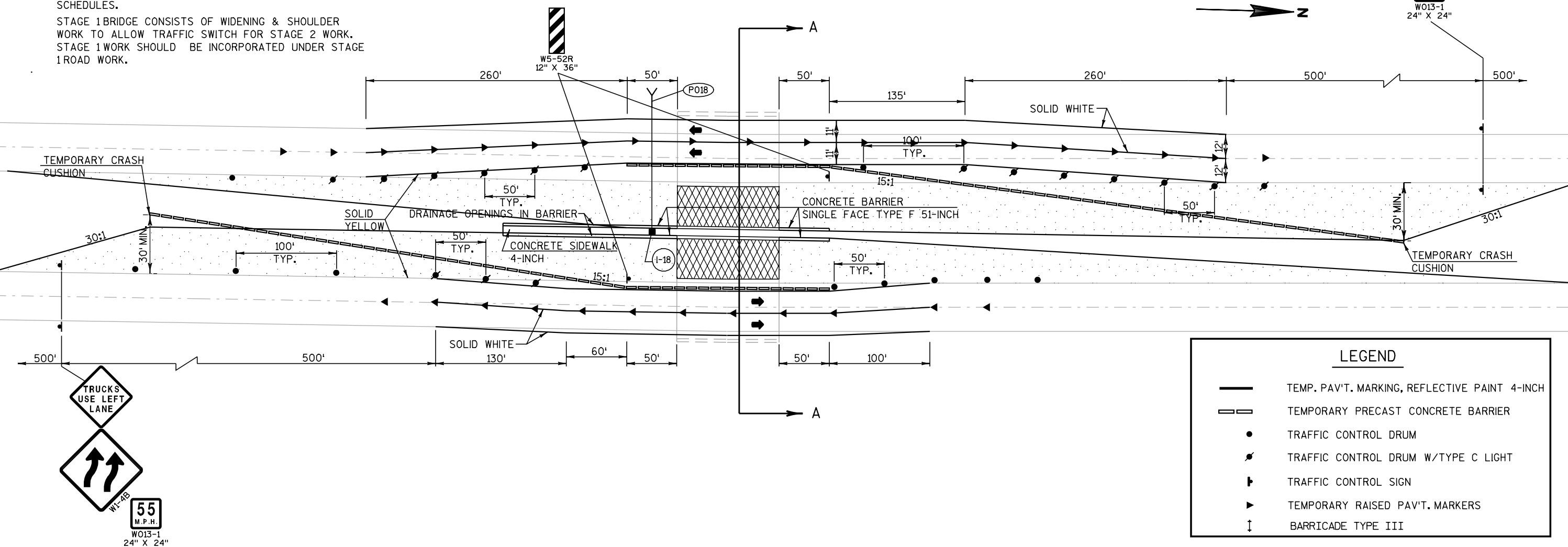
## SECTION A-A STAGE 2 BRIDGE

### BRIDGE WORK NOTES

THE BRIDGE CONTRACTOR SHALL COORDINATE WITH THE ROADWAY CONTRACTOR TO ESTABLISH CONSTRUCTION SCHEDULES.

STAGE 1 BRIDGE CONSISTS OF WIDENING & SHOULDER WORK TO ALLOW TRAFFIC SWITCH FOR STAGE 2 WORK. STAGE 1 WORK SHOULD BE INCORPORATED UNDER STAGE 1 ROAD WORK.

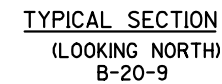
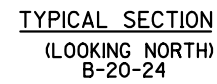
1) CONSTRUCT BRIDGE MEDIAN WIDENING











### LEGEND

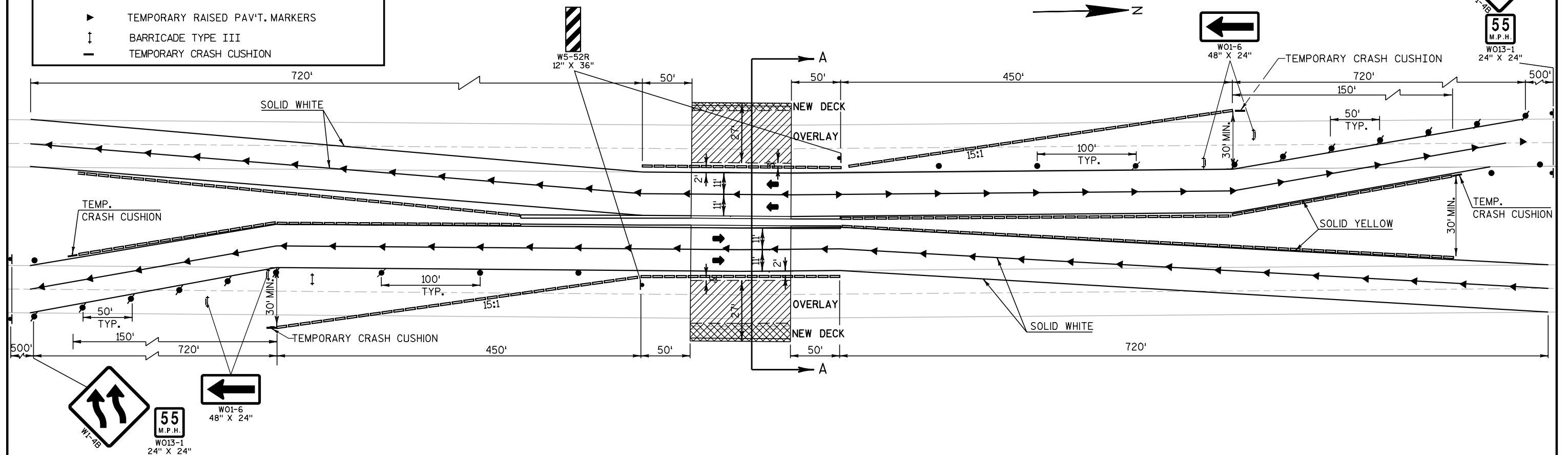
- TEMP. PAV'T. MARKING, REFLECTIVE PAINT 4-INCH
- ▬ TEMPORARY PRECAST CONCRETE BARRIER
- TRAFFIC CONTROL DRUM
- ◊ TRAFFIC CONTROL DRUM W/TYPE C LIGHT
- ⬮ TRAFFIC CONTROL SIGN
- ▶ TEMPORARY RAISED PAV'T. MARKERS
- ⬮ BARRICADE TYPE III

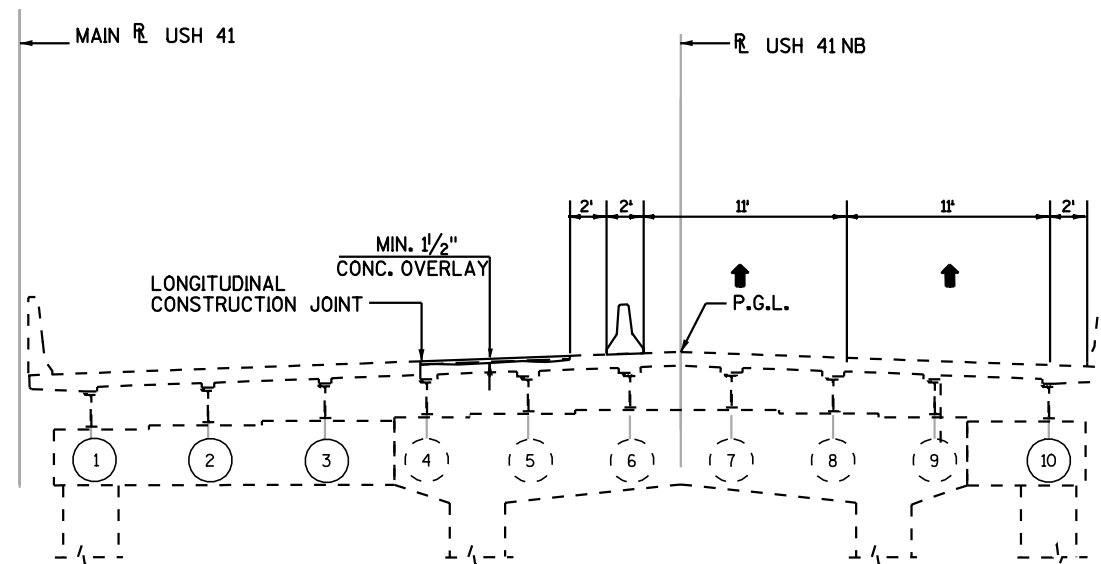
THE BRIDGE CONTRACTOR SHALL COORDINATE WITH THE ROADWAY CONTRACTOR TO ESTABLISH CONSTRUCTION SCHEDULES.



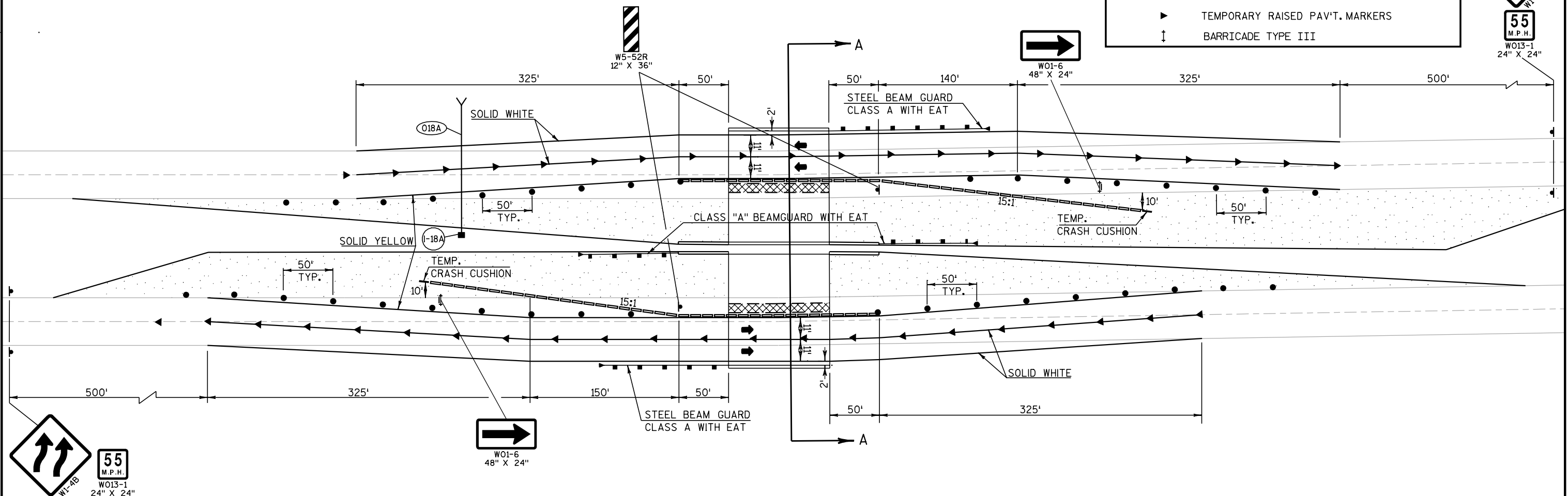
- 1) CONSTRUCT BRIDGE OUTSIDE WIDENING
- 2) OVERLAY OUTER LANES OF EXISTING BRIDGE

- ### LEGEND
- |   |   |
|---|---|
|    | TEMP. PAV'T. MARKING, REFLECTIVE PAINT 4-INCH |
|    | TEMPORARY PRECAST CONCRETE BARRIER            |
|    | TRAFFIC CONTROL DRUM                          |
|    | TRAFFIC CONTROL DRUM W/TYPE C LIGHT           |
|  | TRAFFIC CONTROL SIGN                          |
|  | TEMPORARY RAISED PAV'T. MARKERS               |
|  | BARRICADE TYPE III                            |
|  | TEMPORARY CRASH CUSHION                       |





TYPICAL SECTION  
(LOOKING NORTH)  
B-20-9



THE BRIDGE CONTRACTOR SHALL COORDINATE WITH THE ROADWAY CONTRACTOR TO ESTABLISH CONSTRUCTION SCHEDULES.

|   |   |
|---|---|
|    | TEMP. PAV'T. MARKING, REFLECTIVE PAINT 4-INCH |
|    | TEMPORARY PRECAST CONCRETE BARRIER            |
|    | TRAFFIC CONTROL DRUM                          |
|    | TRAFFIC CONTROL DRUM W/TYPE C LIGHT           |
|    | TRAFFIC CONTROL SIGN                          |
|    | TEMPORARY RAISED PAV'T. MARKERS               |
|  | BARRICADE TYPE III                            |

