

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plot
Section No.	5	Plan and Profile (Including Erosion Control)
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Gross Sections

TOTAL SHEETS = 26



DESIGN DESIGNATION 3626-00-73

A.A.D.T.	2022	=	1800
A.A.D.T.	2042	=	1800
D.H.V.		=	6.8
O.D.		=	60/40
T.		=	8.1
DESIGN SPEED		=	30/60
ESALS		=	240,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

ROCK	
LABEL	
35.36	
18	
31	
E	
FD	
G	
SAN	
SS	
T	
W	
DB	
Ø	

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

TOWN OF DEERFIELD, LIBERTY ROAD

(OAK PARK RD TO ROBERT NELSON RD)

TOWN ROAD  
DANE COUNTY

STATE PROJECT NUMBER

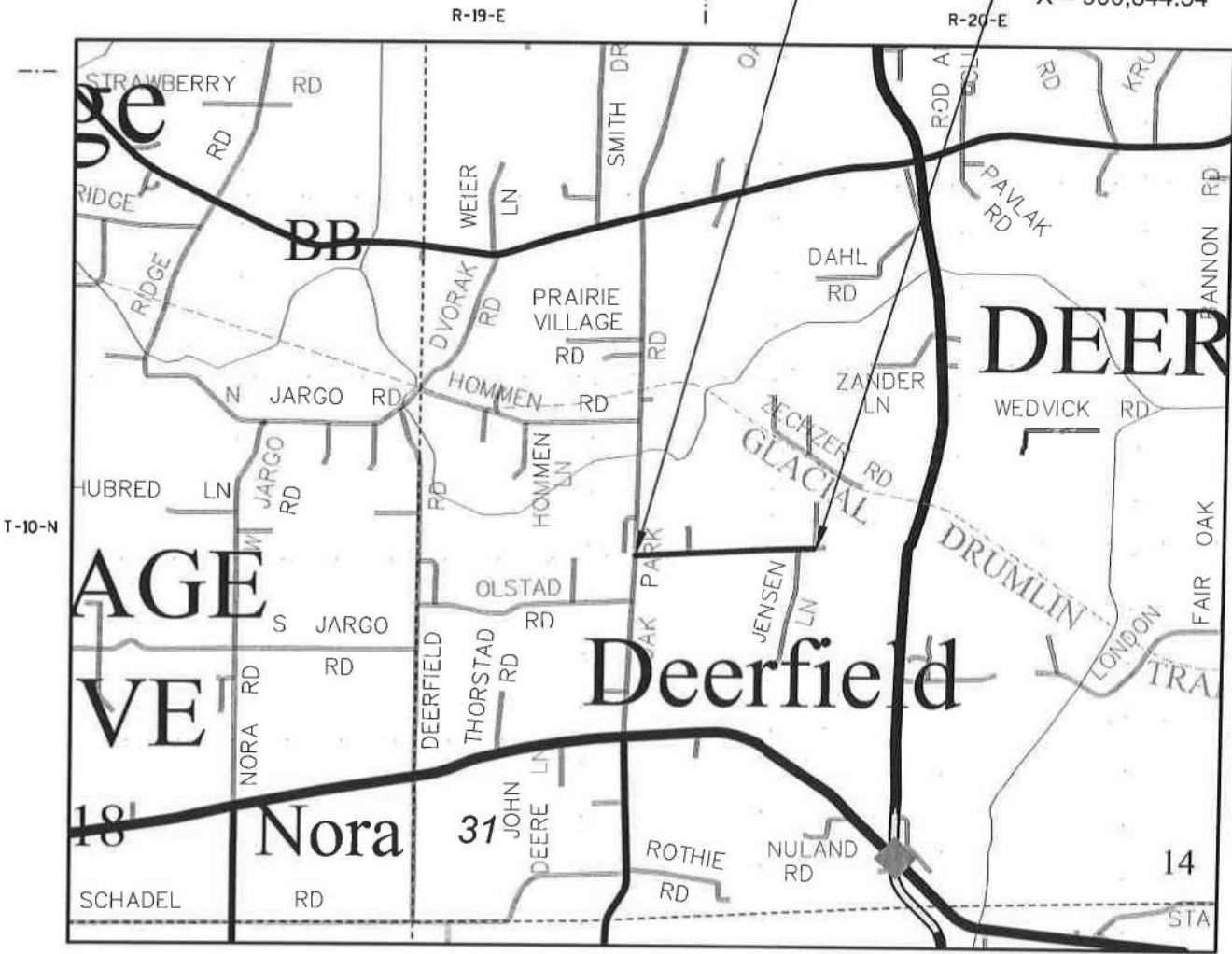
3626-00-73

BEGIN PROJECT 3626-00-73

STA. 11+00  
Y = 474,374.14  
X = 895,539.13

END PROJECT 3626-00-73

STA. 64+12.00  
Y = 474,638.06  
X = 900,844.54



LAYOUT  
SCALE 0 1 MI.

TOTAL NET LENGTH OF CENTERLINE = 1.0065 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, DANE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ELEVATION SHOWN ON THIS PLAN ARE REFERENCE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988, NAVD88 (2012.)

STATE PROJECT

3626-00-73

FEDERAL PROJECT

PROJECT

WISC 2021434

CONTRACT

1

ACCEPTED FOR

TOWN OF DEERFIELD

Date 4-27 Michael Schulte  
CHAIRMAN

ORIGINAL PLANS PREPARED BY

MSA

1702 Pankratz St. Madison, WI 53704  
608-242-7779 1-800-446-0679 Fax: 608-242-5664

WISCONSIN  
CHAD D. WAGNER  
E-45546  
MADISON  
WI  
PROFESSIONAL ENGINEER

DATE: 4/27/2021  
(Professional Engineer Signature)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor N/A  
Designer MSA PROFESSIONAL SERVICES, INC.  
Project Manager TRAVIS G. BUROS, P.E.  
Regional Examiner SW REGION  
Regional Supervisor OSCAR (IAN) WINGER

APPROVED FOR THE DEPARTMENT

DATE: 4/29/2021 Travis G. Buros  
(Signature)

GENERAL NOTES:

APPLY TACK COAT AT A MINIMUM RATE OF 0.07 GAL/SY TO MILLED SURFACES, AND 0.05 GAL/SY BETWEEN HMA LIFTS. HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/INCH.

CONSTRUCTION LIMITS , AS SHOWN ON PLANS, ARE APPROXIMATE LOCATIONS AND MAY BE ADJUSTED BY THE FIELD ENGINEER BASED ON FIELD CONDITIONS.

SAWING ASPHALT IS INCIDENTAL TO REMOVING ASPHALTIC SURFACE BUTT JOINTS BID ITEM.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN THE DRIVING, TURNING, PASSING, OR PARKING LANE.

APPLY TACK COAT TO EXISTING HMA SURFACE, BETWEEN NEW PAVED SURFACES, OR AS DIRECTED BY THE FIELD ENGINEER.

NEW SHOULDER AGGREGATE SHALL INCORPORATE THE EXISTING SHOULDER AGGREGATE UNLESS OTHERWISE DIRECTED BY THE FIELD ENGINEER.

DIMENSIONS GIVEN FOR THE EXISTING FEATURES SHALL BE CONSIDERED APPROXIMATE AND MEASURED IN THE FIELD FOR MATCHING PURPOSES.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, ALL SIGNS RELATING TO THE OPERATION SHALL BE COVERED OR REMOVED AND THE FACILITY RESTORED TO NORMAL OPERATION.

THE LIMITS OF PAYMENT REMOVAL ON SIDE STREETS ARE APPROXIMATE AND WILL BE VERIFIED IN THE FIELD BY THE ENGINEER.

TYPICAL FINISHED SECTIONS SHOW THE GENERAL ROADWAY FEATURES THROUGHOUT THE PROJECT. PAVEMENT SLOPES, BORDER SLOPES, ETC., MAY VARY WITHIN THE STATION LIMITS OF EACH SECTION.

COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

THE EROSION CONTROL ITEMS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. THE ENGINEER WILL DETERMINE THE EXACT LOCATIONS OF EROSION CONTROL ITEMS. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY. REMOVE ITEMS AT THE ENGINEERS DISCRETION.

ADJUST NUMBER, LOCATION, AND SPACING OF TRAFFIC CONTROL SIGNS AND DEVICES, AS SHOWN ON THE PLANS, TO FIT FIELD CONDITIONS.

EXISTING RIGHT OF WAY LINES ARE APPROXIMATE.

STANDARD ABBREVIATIONS

AC	ACRES
AEW	APRON ENDWALL
AGG	AGGREGATE
AH	AHEAD
ALUM.	ALUMINUM
A.P.	ACCESS POINT
ASPH	ASPHALT
AVE	AVENUE
BAD	BASE AGGREGATE DENSE
BK	BACK
BLK	BLOCK
BOC	BACK OF CURB
BOW	BACK OF SIDEWALK
BM	BENCHMARK
CABC	CRUSHED AGGREGATE BASE COURSE
CL or c	CENTERLINE
Δ	CENTRAL ANGLE or DELTA
CMCP	CORRUGATED METAL CULVERT PIPE
CONC	CONCRETE
CP	CONTROL POINT
CPCS	CULVERT PIPE CORRUGATED STEEL
CSM	CERTIFIED SURVEY MAP
CTH	COUNTY TRUNK HIGHWAY
D	DEGREE OF CURVATURE
DES	DESIRABLE
E	EAST
EB	EASTBOUND
EBS	EXCAVATION BELOW SUBGRADE
EOP	EDGE OF PAVEMENT
ET AL	AND OTHERS
EW	ENDWALL
EXIST	EXISTING
FT	FOOT
FT2	SQUARE FEET
GN	GRID NORTH
GV	GAS VALVE
HERCP	HORIZONTAL ELLIPTICAL REINFORCED CONCRETE PIPE
HYD	HYDRANT
IN	INCH
INL	INLET
INV	INVERT
IP	IRON PIPE

L	LENGTH
L	LENGTH OF CURVE
LC	LONG CHORD
LCP	LONG CHORD BEARING
LF	LINEAR FEET
LT	LEFT
MH	MANHOLE
MON	MONUMENT
N	NORTH
NB	NORTHBOUND
N.C.	NORMAL CROWN
NO	NUMBER
PB	PULLBOX
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
PL	PROPERTY LINE
PLE	PERMANENT LIMITED EASEMENT
POB	POINT OF BEGINNING
PT	POINT OF TANGENCY
R	RADIUS
R	RANGE
RCP	REINFORCED CONCRETE PIPE
RD	ROAD
REQ'D	REQUIRED
RL or R/L	REFERENCE LINE
RP	RADIUS POINT
RT	RIGHT
R/W	RIGHT OF WAY
S	SOUTH
SAN	SANITARY SEWER
SB	SOUTHBOUND
SDD	STANDARD DETAIL DRAWING
S.E.	SUPERELEVATION
SEC	SECTION
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
SSPRCHE	STORM SEWER PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL
SQ	SQUARE
ST	STREET
STA	STATION
STD	STANDARD
STH	STATE TRUNK HIGHWAY
STM	STORM SEWER

STR	STRUCTURE
T	TANGENT
TAN	TANGENT
TEMP	TEMPORARY
TLE	TEMPORARY LIMITED EASEMENT
T or TN	TOWN
TYP.	TYPICAL
WM	WATERMIN
WV	WATER VALVE
W	WEST
WB	WESTBOUND
X	EAST GRID COORDINATE
Y	NORTH GRID COORDINATE

DNR LIAISON

WISCONSIN DEPARTMENT OF NATURAL RESOURCES  
ATTN: ERIC HEGGELUND  
3911 FISH HATCHERY ROAD  
FITCHBURG, WI 53711  
PHONE: (608) 275-3301  
EMAIL: eric.heggelund@wisconsin.gov

DESIGN CONTACTS

WISCONSIN DEPARTMENT OF TRANSPORTATION  
SOUTHWEST REGION  
ATTN: TRAVIS BUROS, P.E.  
3350 MORMON COULEE ROAD  
LA CROSSE, WI 54601  
PHONE: (608) 789-5702  
EMAIL: travis.buros@dot.wi.gov

MSA PROFESSIONAL SERVICES, INC.  
ATTN: CHAD WAGNER, P.E.  
1702 PANKRATZ ST  
MADISON, WI 53704  
PHONE: (608) 242-6651  
E-MAIL: cwagner@msa-ps.com

COMMUNICATIONS

CHARTER SPECTRUM  
CONTACT: KYLE SMITH  
1348 PLAINFIELD AVEUNE  
JANESVILLE, WI 53545  
PHONE: (608) 575-1937  
EMAIL: kyle.smith@charter.com

COMMUNICATIONS

FRONTIER  
CONTACT: JERRY MOORE  
222 WEST WISCONSIN STREET  
PORTAGE, WI 53901  
CONTACT: JERRY MOORE  
PHONE: (608) 742-9507  
EMAIL: jerald.r.moore@ftr.com

ELECTRIC

WE ENERGIES  
CONTACT: ERIC KICKHAVER  
500 S 116TH ST  
WEST ALLIS, WI 53214  
PHONE: (414) 944-5917  
EMAIL: eric.kickhaver@we-energies.com

GAS

ALLIANT ENERGY  
CONTACT: BEN BLOYER  
1521 PROGRESS LANE  
STOUGHTON, WI 53589  
PHONE: (608) 501-4298  
EMAIL: benjaminbloyer@alliantenergy.com

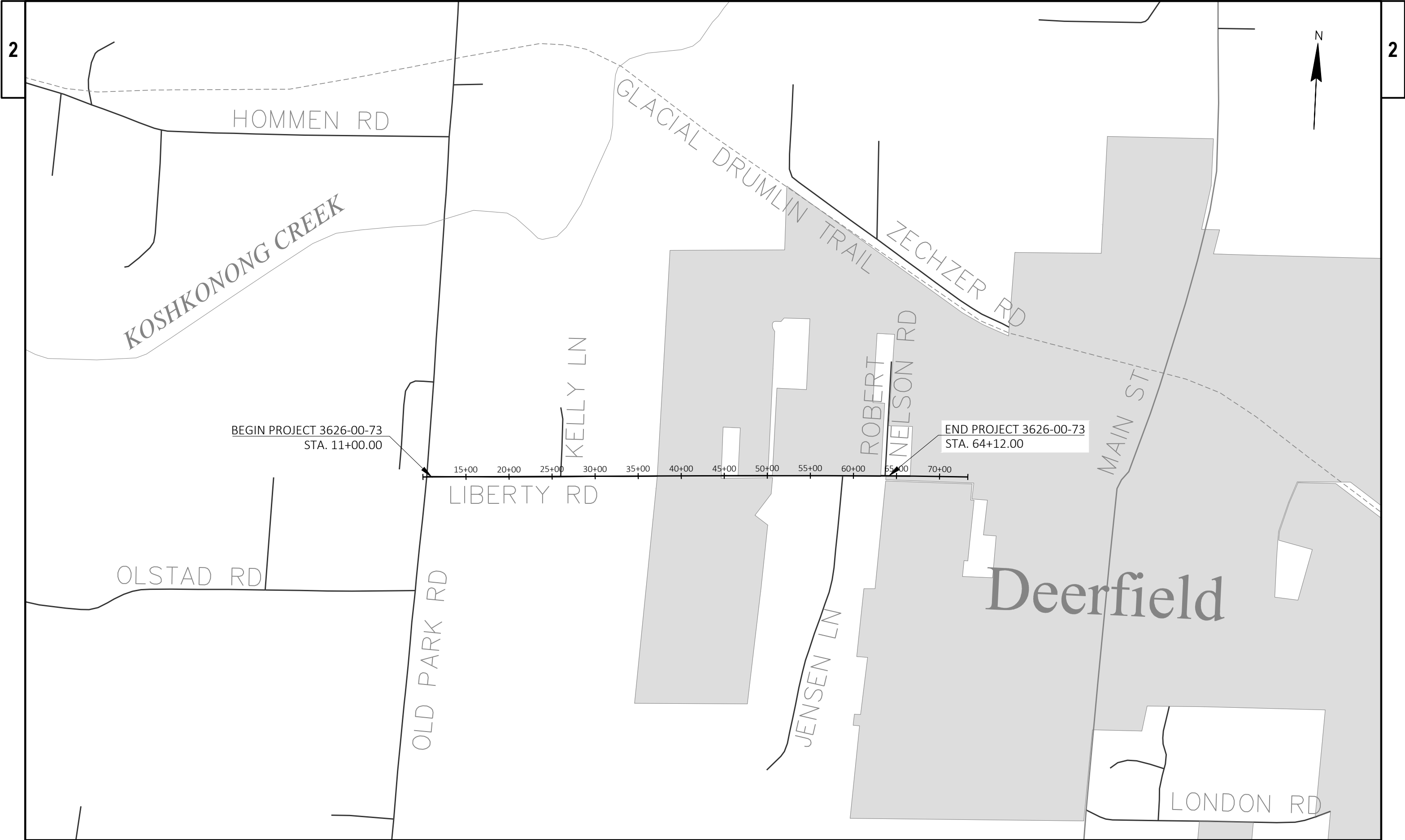
WATER

VILLAGE OF DEERFIELD  
CONTACT: JOHN DOYLE  
205 N. INDUSTRIAL PARK DRIVE  
DEERFIELD, WI 53531  
PHONE: (608) 764-5497  
EMAIL: jdoyle@deerfieldwi.com

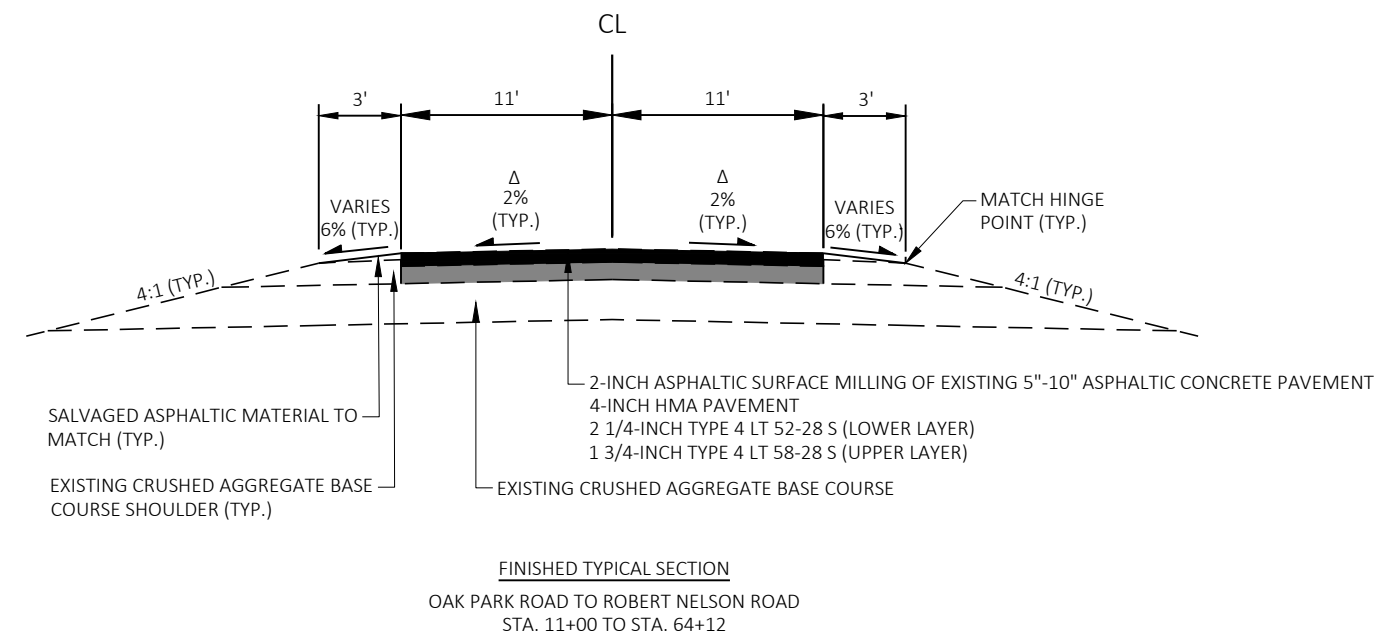
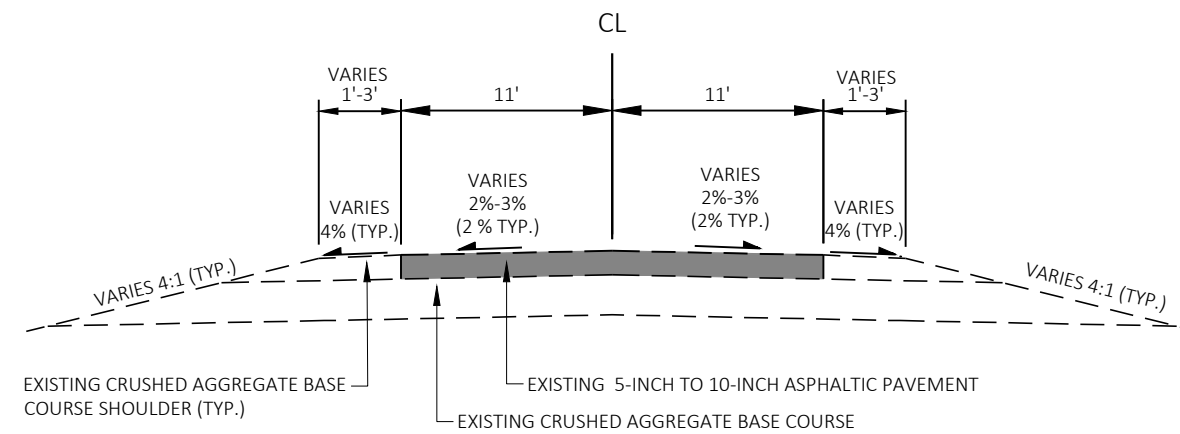
INDEX

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- TRAFFIC CONTROL OVERVIEW

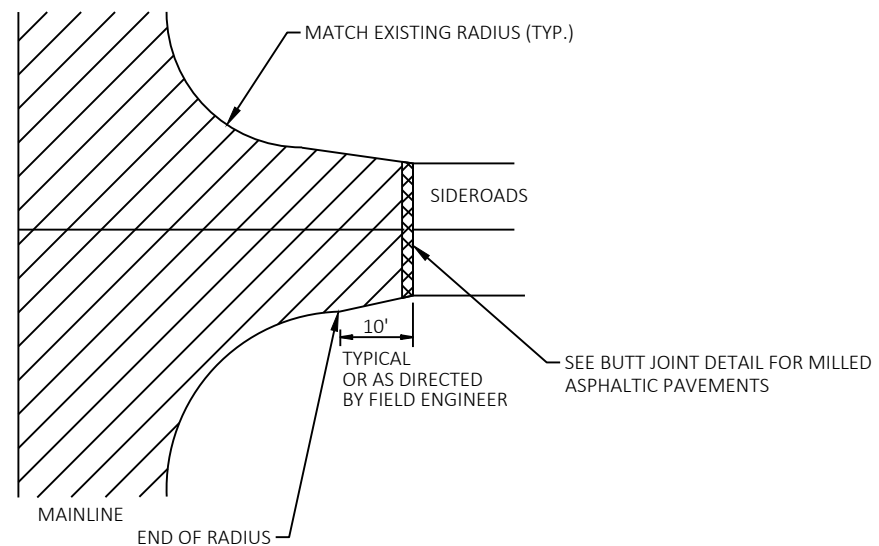




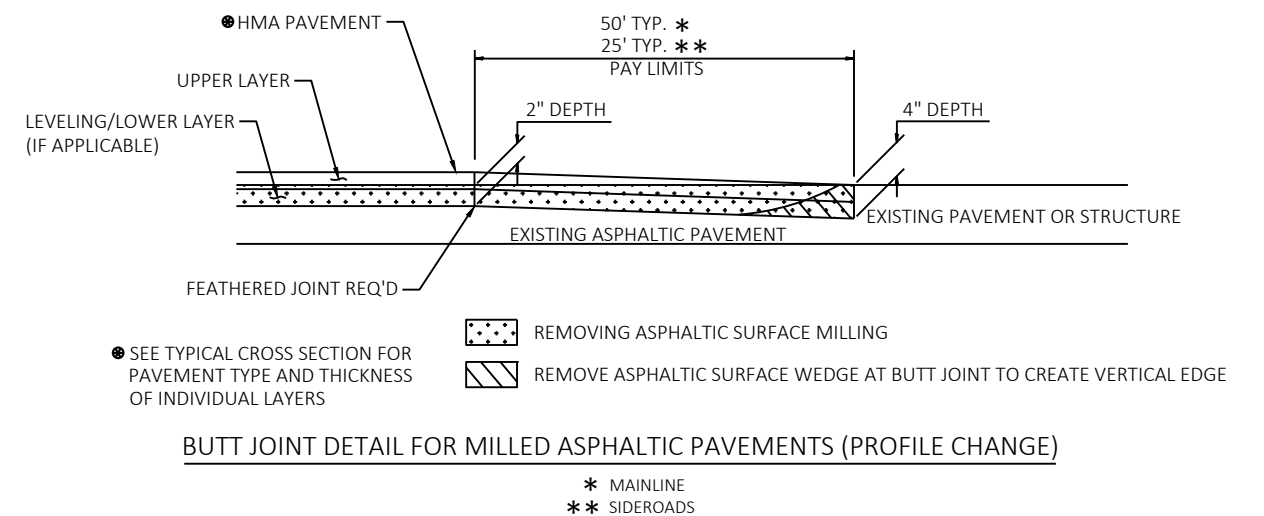
PROJECT NO: 3626-00-73	HWY: LIBERTY RD	COUNTY: DANE	PROJECT OVERVIEW	SHEET	E
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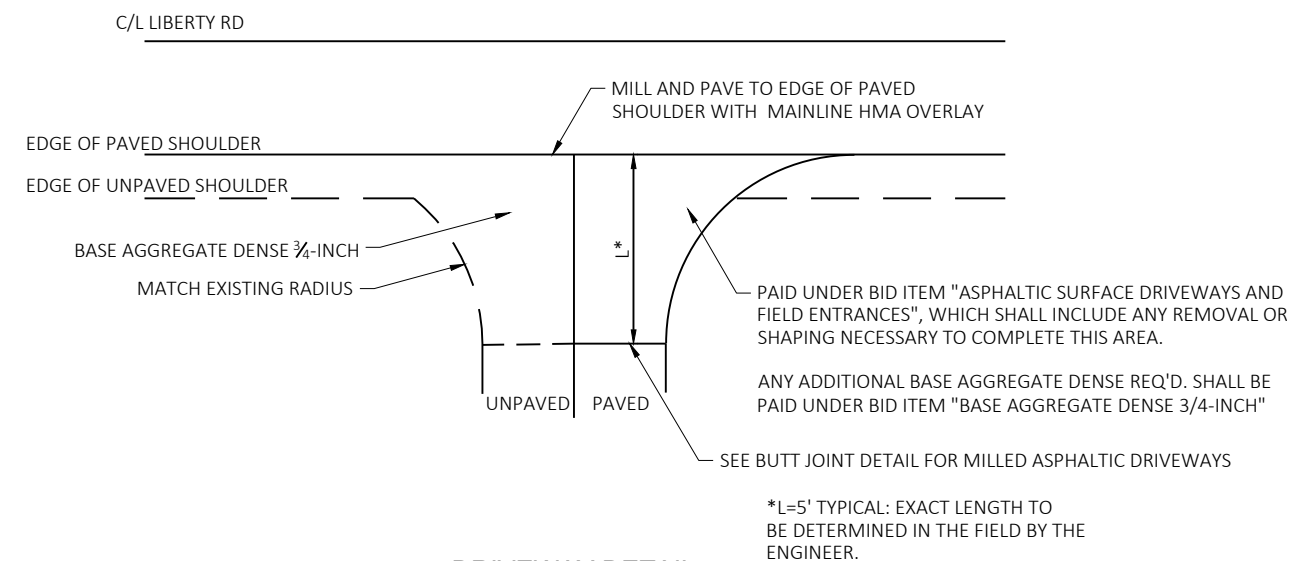


NOTES:  
Δ MATCH EXISTING CROSS SLOPE



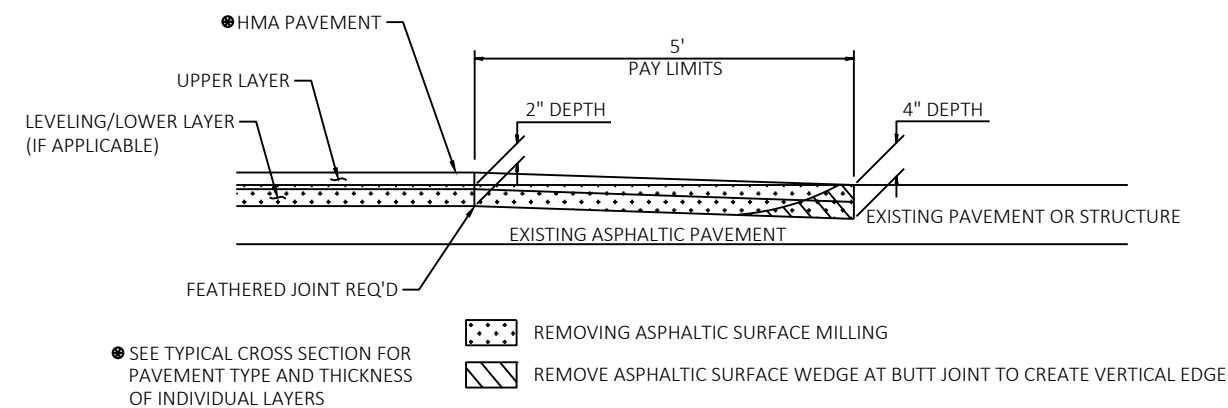
BUTT JOINT DETAIL FOR INTERSECTIONS



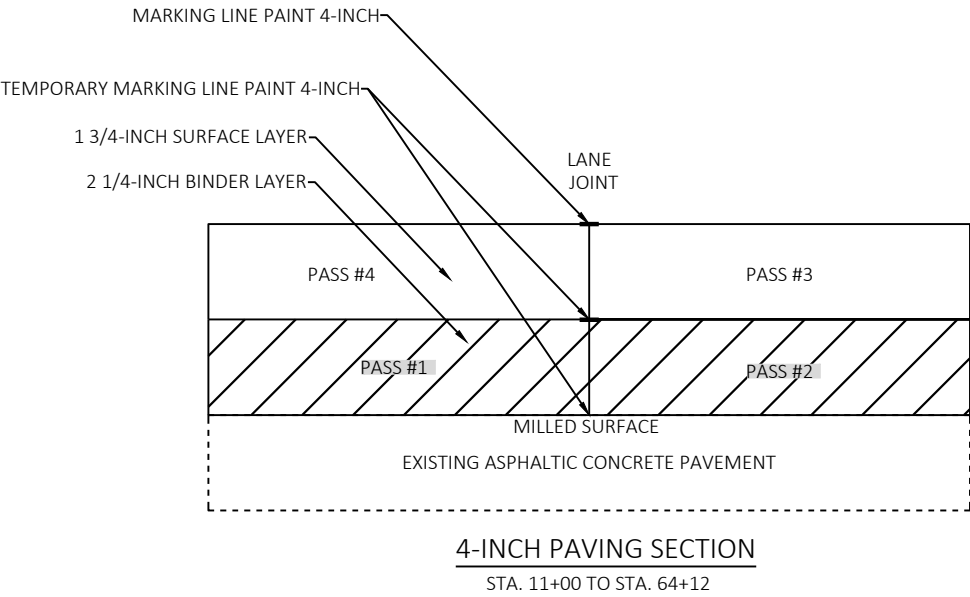


**DRIVEWAY DETAIL**

ASPH	BAD
STA 15+76	STA 23+46
STA 35+71	STA 34+04
STA 45+45	STA 50+52
STA 51+57	STA 50+94

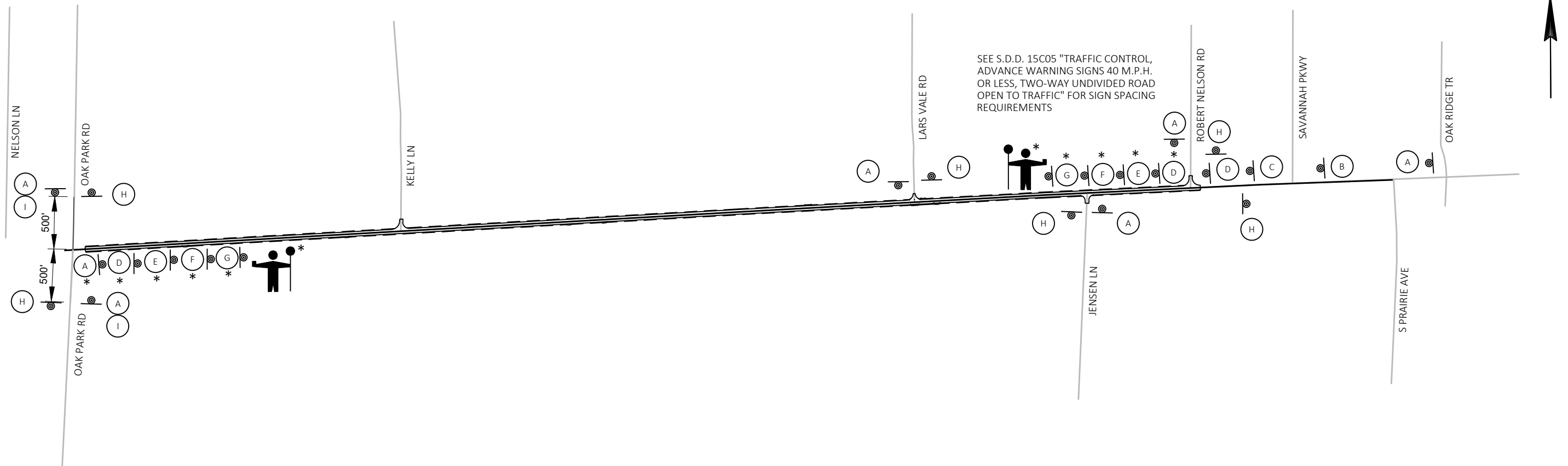
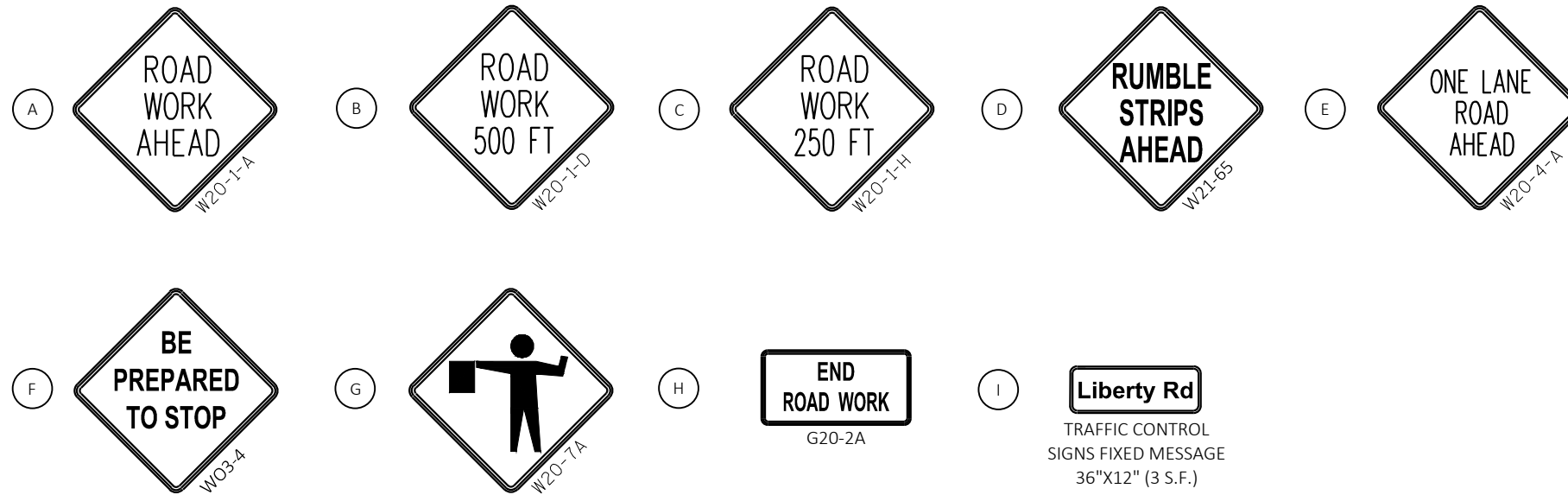


**BUTT JOINT DETAIL FOR MILLED ASPHALTIC DRIVEWAYS**



- NOTES:
- 1. TO BE UTILIZED FOR TEMPORARY INSTALLATIONS IN WORK AREAS INVOLVING PAVING LONGITUDINAL JOINTS.
  - 2. UNEVEN LANE SIGNS WILL BE PLACED AT THE BEGINNING OF JOINT AND AT 1/2 MILE INCREMENTS.
  - 3. SIGNS WILL BE PAID AS TRAFFIC CONTROL SIGNS.
  - 4. REFER TO SDD 15D39 "TRAFFIC CONTROL, DROP-OFF SIGNING" FOR ADDITIONAL INFORMATION.

LONGITUDINAL JOINTS DETAIL





Estimate Of Quantities

3626-00-73					
Line	Item	Item Description	Unit	Total	Qty
0002	204.0115	Removing Asphaltic Surface Butt Joints	SY	820.000	820.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	12,745.000	12,745.000
0006	211.0100	Prepare Foundation for Asphaltic Paving (project) 01. 3626-00-73	LS	1.000	1.000
0008	213.0100	Finishing Roadway (project) 01. 3626-00-73	EACH	1.000	1.000
0010	305.0110	Base Aggregate Dense 3/4-Inch	TON	25.000	25.000
0012	305.0504.S	Hauling Excess Shoulder Material	CY	50.000	50.000
0014	455.0605	Tack Coat	GAL	1,558.000	1,558.000
0016	460.2000	Incentive Density HMA Pavement	DOL	1,870.000	1,870.000
0018	460.5224	HMA Pavement 4 LT 58-28 S	TON	2,909.000	2,909.000
0020	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	26.000	26.000
0022	618.0100	Maintenance And Repair of Haul Roads (project) 01. 3626-00-73	EACH	1.000	1.000
0024	619.1000	Mobilization	EACH	1.000	1.000
0026	624.0100	Water	MGAL	5.000	5.000
0028	642.5001	Field Office Type B	EACH	1.000	1.000
0030	643.0900	Traffic Control Signs	DAY	338.000	338.000
0032	643.1000	Traffic Control Signs Fixed Message	SF	6.000	6.000
0034	643.1050	Traffic Control Signs PCMS	DAY	21.000	21.000
0036	643.5000	Traffic Control	EACH	1.000	1.000
0038	646.1005	Marking Line Paint 4-Inch	LF	20,336.000	20,336.000
0040	646.6105	Marking Stop Line Paint 18-Inch	LF	11.000	11.000
0042	648.0100	Locating No-Passing Zones	MI	1.010	1.010
0044	649.0105	Temporary Marking Line Paint 4-Inch	LF	20,032.000	20,032.000
0046	650.8000	Construction Staking Resurfacing Reference	LF	5,300.000	5,300.000
0048	650.9910	Construction Staking Supplemental Control (project) 01. 3626-00-73	LS	1.000	1.000
0050	740.0440	Incentive IRI Ride	DOL	4,015.000	4,015.000
0052	SPV.0060	Special 01. Verify Landmark Reference Monuments	EACH	2.000	2.000
0054	SPV.0060	Special 02. Landmark Reference Monuments Special	EACH	2.000	2.000
0056	SPV.0195	Special 01. Salvaged Asphaltic Material	TON	650.000	650.000

REMOVING ASPHALTIC SURFACE BUTT JOINTS					
(204.0115) REMOVING ASPHALTIC SURFACE BUTT JOINTS					
CATEGORY	STATION	TO STATION	LOCATION	SY	NOTES
0010	11+00	11+50	LT & RT	122	BEGIN OF PROJECT TRANSITION
	15+62	15+90	RT	23	PRIVATE DRIVEWAY
	25+60	26+46	LT	139	KELLY LN
	35+57	35+85	RT	32	PRIVATE DRIVEWAY
	45+26	45+64	LT	34	PRIVATE DRIVEWAY
	50+16	50+83	LT	78	LARS VALE RD
	51+44	51+69	RT	19	PRIVATE DRIVEWAY
	58+37	59+06	RT	106	JENSEN LN
	63+25	64+12	LT	143	ROBERT NELSON RD
	63+62	64+12	LT & RT	122	END PROJECT TRANSITION
TOTAL				820	

REMOVING ASPHALTIC SURFACE MILLING				
(204.0120) REMOVING ASPHALTIC SURFACE MILLING				
CATEGORY	STATION	TO STATION	LOCATION	SY
0010	11+50	63+62	LT & RT	12,745
TOTAL				12,745

BASE AGGREGATE			
(305.0110) BASE AGGREGATE DENSE 3/4-INCH			
CATEGORY	STATION	LOCATION	TON
0010	23+46	RT	3
	34+04	RT	3
	50+52	RT	5
	50+94	LT	3
UNDISTRIBUTED			12
TOTALS			25

WATER		
(624.0100)		
CATEGORY	DESCRIPTION	MGAL
0010	PROJECT 3626-00-73	5
TOTAL		5
NOTE: WATER BID ITEMS IS FOR BASE AGGREGATE DENSE DUST CONTROL AND COMPACTION.		

SALVAGED ASPHALTIC MATERIAL					
*					
				SPV.0195.01 SALVAGED ASPHALTIC MATERIAL	(305.0504.S) HAULING EXCESS SHOULDER MATERIAL
CATEGORY	STATION	TO STATION	LOCATION	TON	CY
0010	11+00	64+12	LT	295	-
	11+00	64+12	RT	295	-
	UNDISTRIBUTED			60	50
TOTALS				650	50
*QUANTITY ASSUMES EXISTING SHOULDER IS LOW AND 3" OF SALV ASPHALTIC MATERIAL WILL BE NEEDED					

HMA PAVEMENT						
			# (460.5224) UPPER LAYER		# (460.5224) LOWER LAYER	(455.0605) TACK COAT
			HMA PAVEMENT 4 LT 58-28 S		HMA PAVEMENT 4 LT 58-28 S	
CATEGORY	STATION	TO STATION	LOCATION	TON	TON	GAL
0010	11+00	64+12	LT & RT	1,273	1,636	1,558
TOTALS				1,273	1,636	1,558

# Tonnage is eligible for Incentive Density HMA Pavement (460.2000)

ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES				
(465.0120) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES				
CATEGORY	STATION	LOCATION	DESCRIPTION	TON
0010	15+76	RT	PRIVATE DRIVEWAY	6
	35+71	RT	PRIVATE DRIVEWAY	8
	45+45	LT	PRIVATE DRIVEWAY	8
	51+57	RT	PRIVATE DRIVEWAY	5
TOTAL				26

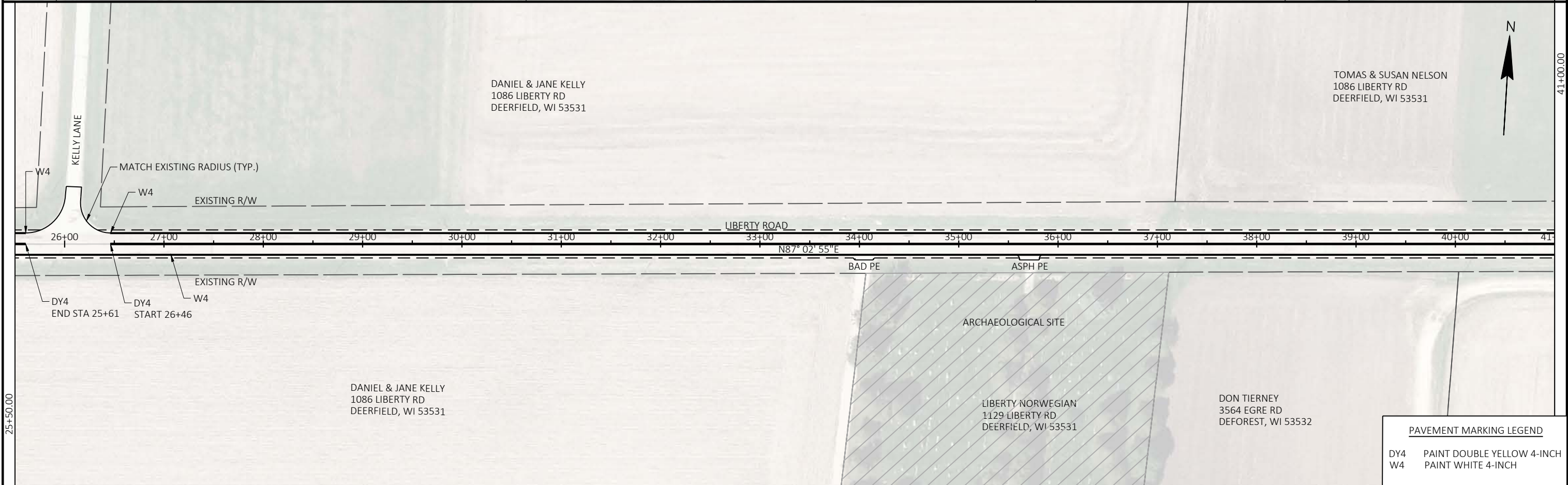
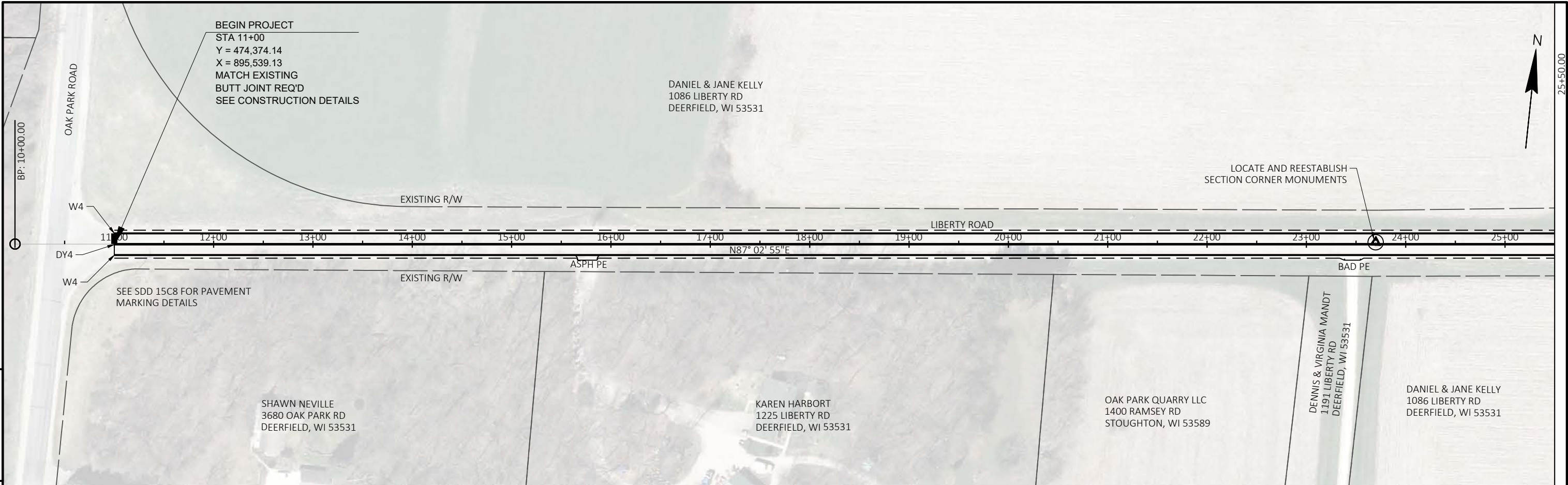
TRAFFIC CONTROL								
		(643.5000)			(643.0900)			
		TRAFFIC CONTROL			TRAFFIC CONTROL	SIGNS FIXED MESSAGE	SIGNS FIXED MESSAGE	
CATEGORY	DESCRIPTION	EACH	SIGNS NO. DEVICES		SIGNS DAY	NO.DEVICES	SF	DURATION
0010	ADVANCE WARNING SIGNS (MAINLINE & SIDEROAD)	-	14		182	-	-	13 DAYS
	DROP-OFF SIGNING	-	2		22	-	-	11 DAYS
	LANE CLOSURE WITH FLAGGING OPERATION	-	9		72	-	-	8 DAYS
	MOVING PAVEMENT MARKING OPERATION	-	4		16	-	-	4 DAYS
	MILLED SURFACE	-	9		36	-	-	4 DAYS
	UNEVEN LANES	-	2		10	-	-	5 DAYS
	PROJECT 3576-01-71	1	-		-	2	6	13 DAYS
TOTALS		1			338	2	6	

NOTES:  
ADVANCE WARNING SIGNS (MAINLINE & SIDEROAD) - SEE SDD 15C05 "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC"  
LANE CLOSURE WITH FLAGGING OPERATION - SEE SDD 15C12 "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION"  
MOVING PAVEMENT MARKING OPERATION - SEE SDD 15C19-a "MOVING PAVEMENT MARKING OPERATION - TWO-LANE TWO-WAY ROADWAY"  
DROP-OFF SIGNING - SEE SDD 15D39 "TRAFFIC CONTROL, DROP-OFF SIGNING"  
MILLED SURFACE - SEE SDD 15D44 "SIGNING ON ROADWAYS WITH MILLED SURFACES"

TRAFFIC CONTROL SIGNS PCMS			
CATEGORY	LOCATION	DESCRIPTION	(643.1050) DAY
0010	OAK PARK RD SOUTHBOUND, NORTH OF PROJECT LIMITS	ADVANCE PROJECT NOTIFICATION	7
	OAK PARK RD NORTHBOUND, SOUTH OF PROJECT LIMITS	ADVANCE PROJECT NOTIFICATION	7
	LIBERTY RD WESTBOUND, EAST OF PROJECT LIMITS	ADVANCE PROJECT NOTIFICATION	7
TOTAL			21

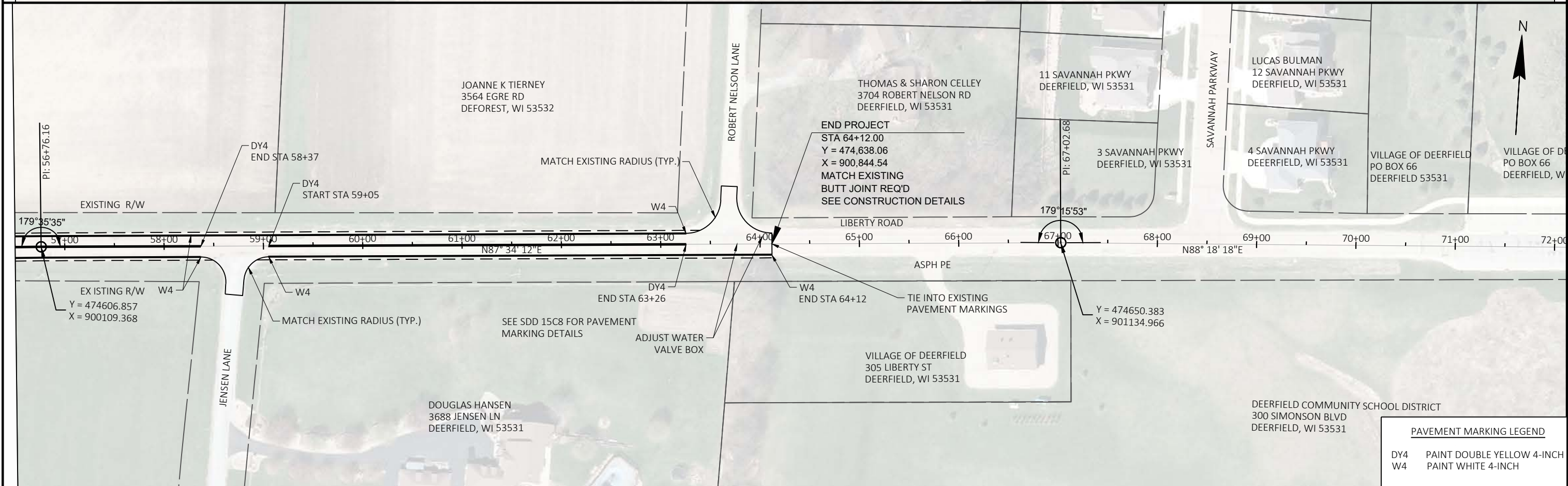
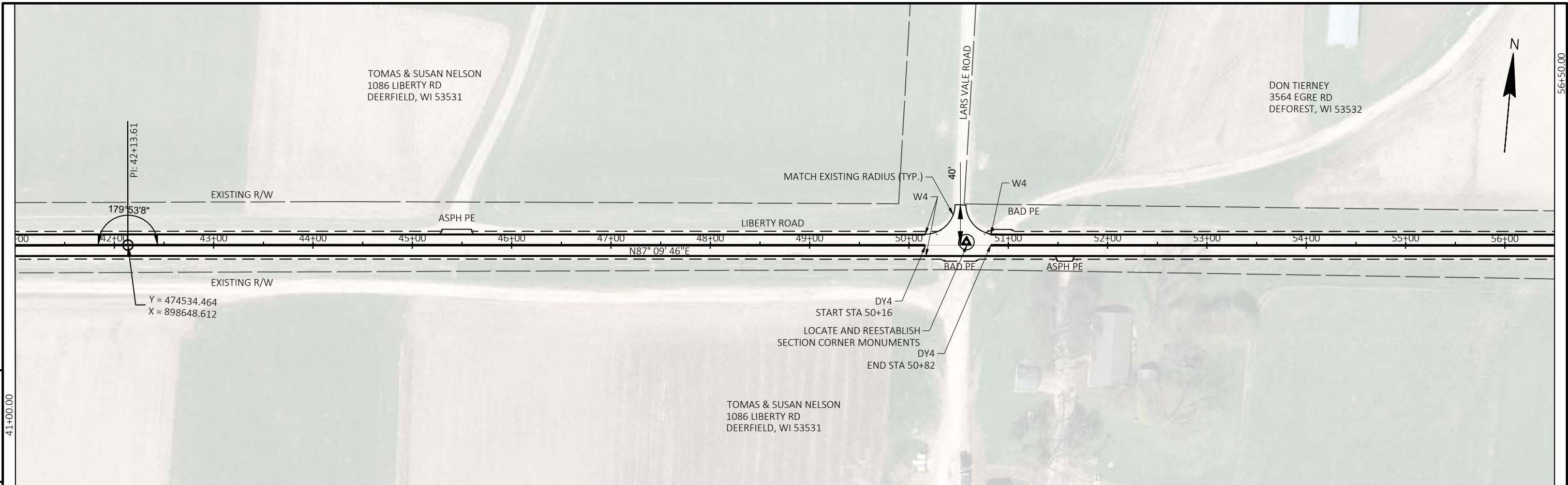
PAVEMENT MARKINGS							
		(646.1005)	(646.1005)	(646.6105)	(649.0105)		
		MARKING LINE PAINT 4-INCH (YELLOW)	MARKING LINE PAINT 4-INCH (WHITE)	MARKING STOP LINE PAINT 18-INCH (WHITE)	TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW)		
CATEGORY	STATION	TO STATION	LF	LF	LF	LF	Comments
0010	11+00	11+00	-	-	11	-	Stop bar at Liberty Rd and Old Park Rd
	11+00	25+61	2922	1461	-	5844	Old Park Rd to Kelly Ln Intersection - Edge line (LT) and Centerline
	11+00	58+37	-	4737	-	-	Old Park Rd to Jensen Ln- Edge line (RT)
	26+46	50+16	4742	2371	-	9484	Kelly Ln to Lars Vale Rd - Edge line (LT) and Centerline
	50+82	58+37	1510	-	-	3020	Robert Nelson Rd - Centerline
	50+82	63+26	-	1244	-	-	Robert Nelson Rd - Edge line (LT)
	59+05	63+26	842	-	-	1684	Jensen Ln to Robert Nelson Rd - Centerline
	59+05	64+12	-	507	-	-	Robert Nelson Rd - Edge line (RT)
TOTALS			10,016	10,320	11	20,032	

REFERENCE MONUMENTS			
		VERIFY LANDMARK REFERENCE MONUMENTS (SPV.0060.01)	LANDMARK REFERENCE MONUMENTS SPECIAL (SPV.0060.02)
CATEGORY	STATION	EACH	EACH
0010	23+70	1	1
	50+60	1	1
		2	2



PROJECT NO: 3626-00-73	HWY: LIBERTY ROAD	COUNTY: DANE	PLAN DETAIL	SHEET	E
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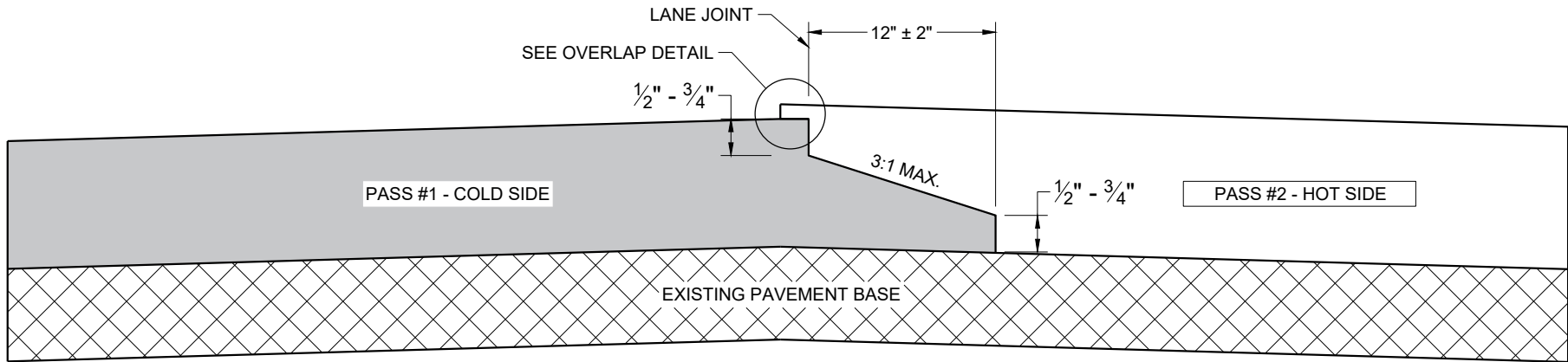


PAVEMENT MARKING LEGEND	
DY4	PAINT DOUBLE YELLOW 4-INCH
W4	PAINT WHITE 4-INCH

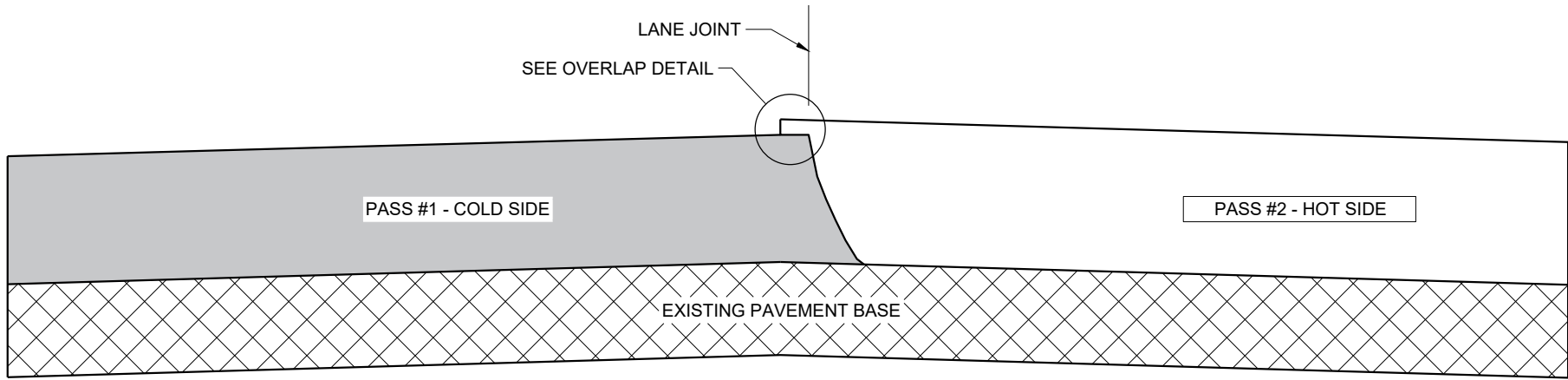
PROJECT NO: 3626-00-73	HWY: LIBERTY ROAD	COUNTY: DANE	PLAN DETAIL	SHEET	E
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Standard Detail Drawing List

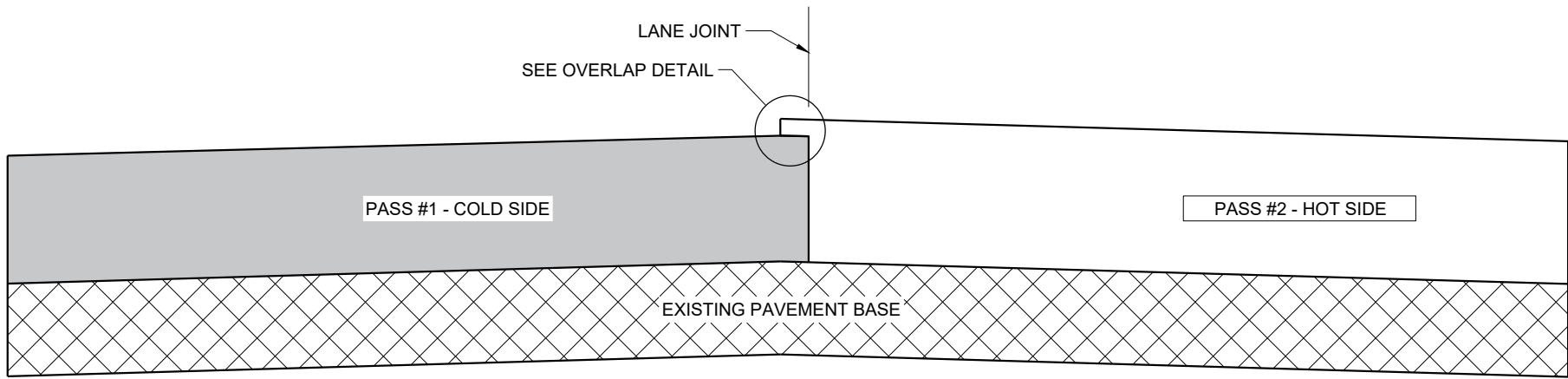
13C19-03	HMA LONGI TUDI NAL JOI NTS
14B29-01	SAFETY EDGE
15C05-05	TRAFFI C CONTROL, ADVANCE WARNING SIGN S 40 M. P. H. OR LESS
15C08-20A	LONGI TUDI NAL MARKI NG (MAINLINE)
15C12-07	TRAFFI C CONTROL FOR LANE CLOSURE WI TH FLAGGI NG OPERATI ON
15C19-06A	MOVI NG PAVEMENT MARKI NG OPERATI ON TWO-LANE TWO-WAY ROADWAY
15D38-02A	TEMPORARY TRAFFI C CONTROL SIGN MOUNTI NG
15D38-02B	ATTACHMENT OF SIGN S TO POSTS
15D39-02	TRAFFI C CONTROL, DROP-OFF SIGNI NG
15D44-02	TRAFFI C CONTROL, SIGNI NG ON ROADWAYS WI TH MI LLED SURFACES



TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)

GENERAL NOTES

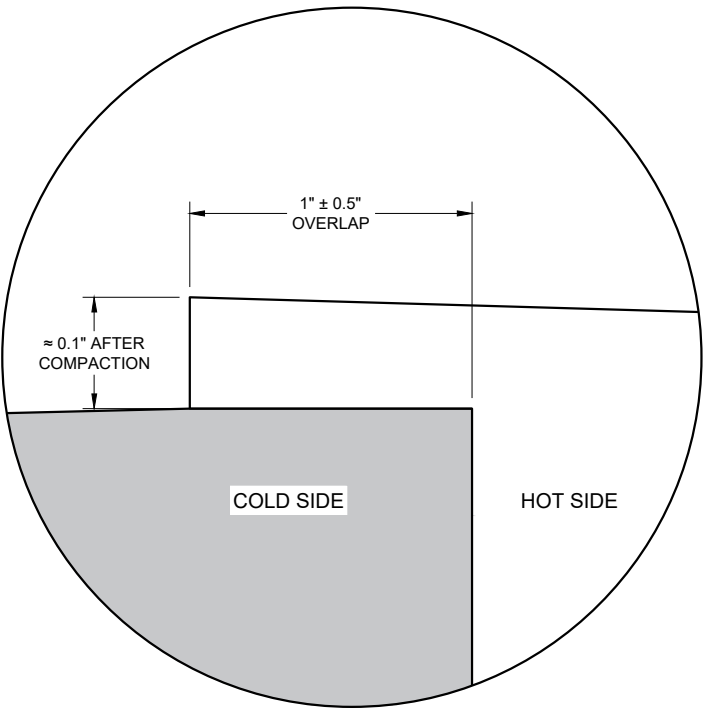
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.

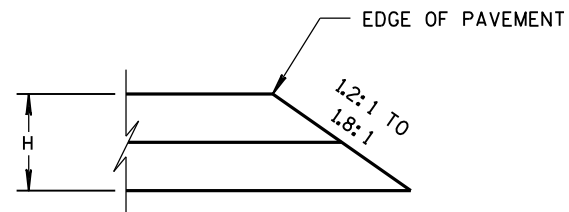


OVERLAP DETAIL (TYPICAL)

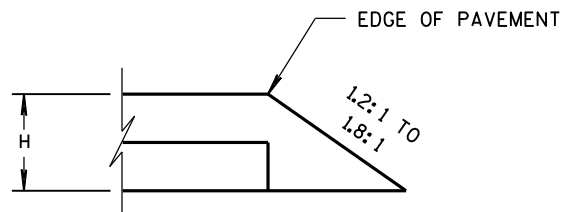
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

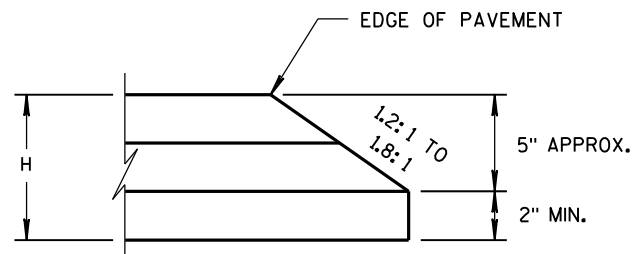
APPROVED  
November 2020 /S/ Steven Hefel  
DATE HMA PAVEMENT ENGINEER  
FHWA



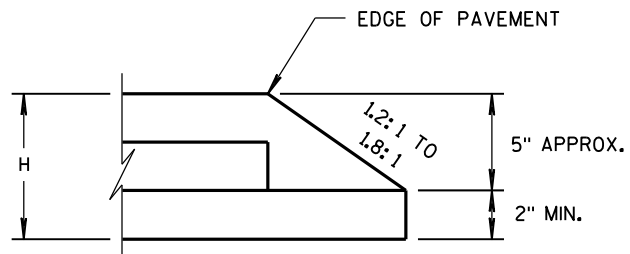
CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER  
FOR H 5" OR LESS

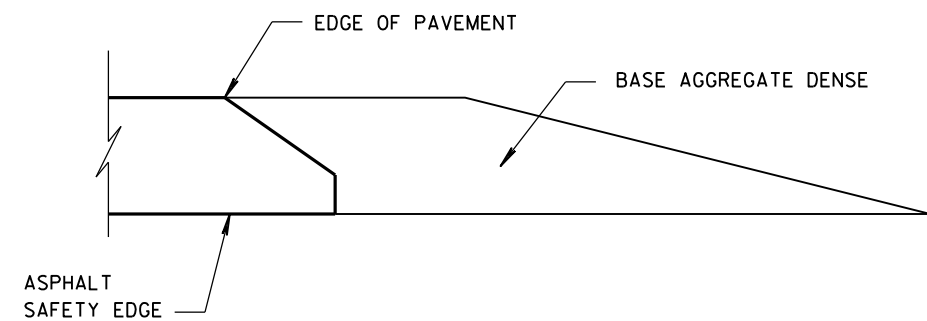


CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER  
FOR H GREATER THAN 5"

### HMA PAVEMENT AND HMA OVERLAYS



### FINISHED SHOULDER AGGREGATE PLACEMENT

SAFETY EDGE<sub>SM</sub>

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

11/30/2012  
DATE

FHWA

/s/ Jerry H. Zogg  
ROADWAY STANDARDS DEVELOPMENT  
ENGINEER



GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

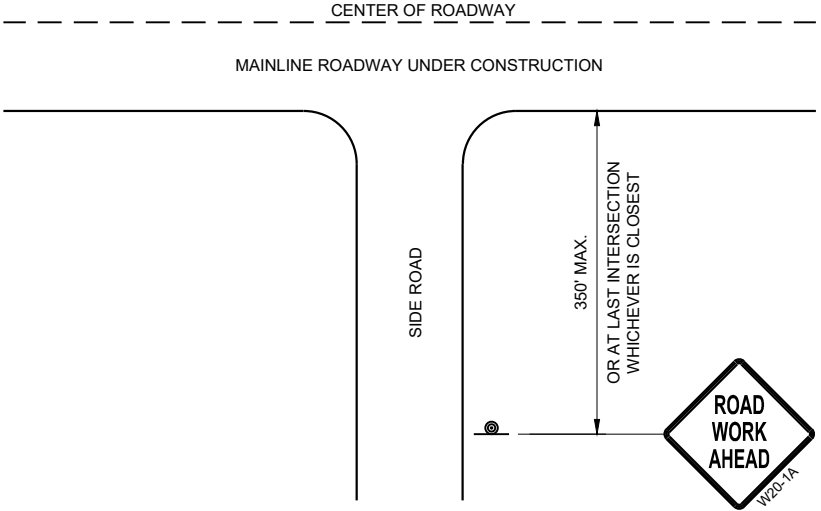
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

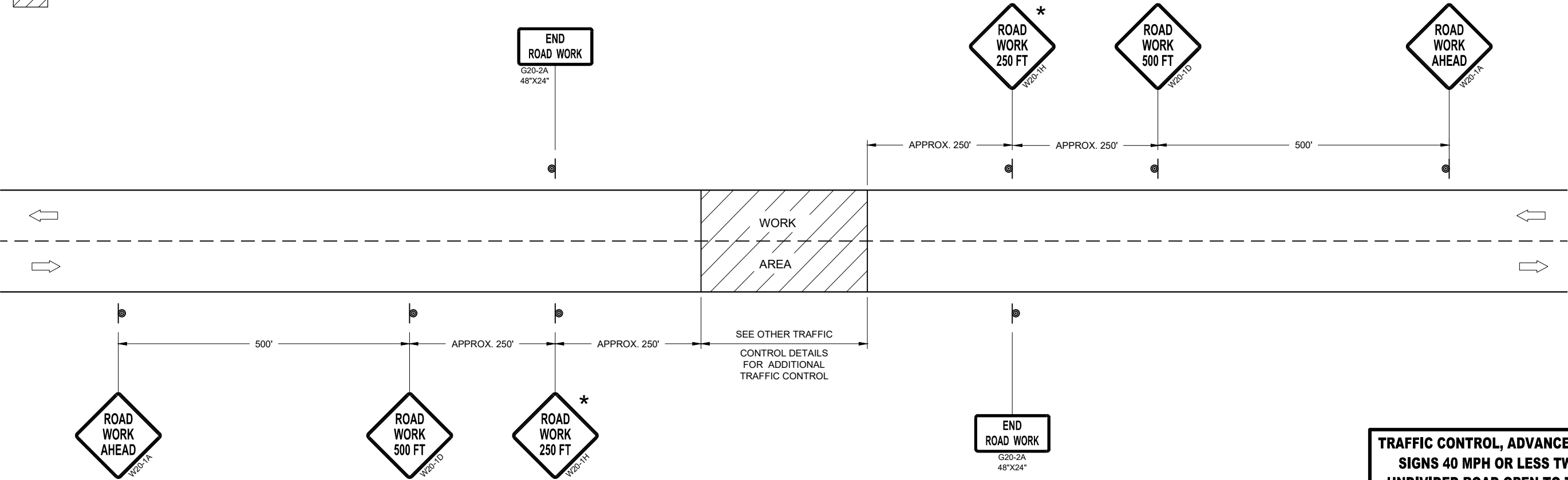
\* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH  
WARNING SIGN DETAIL



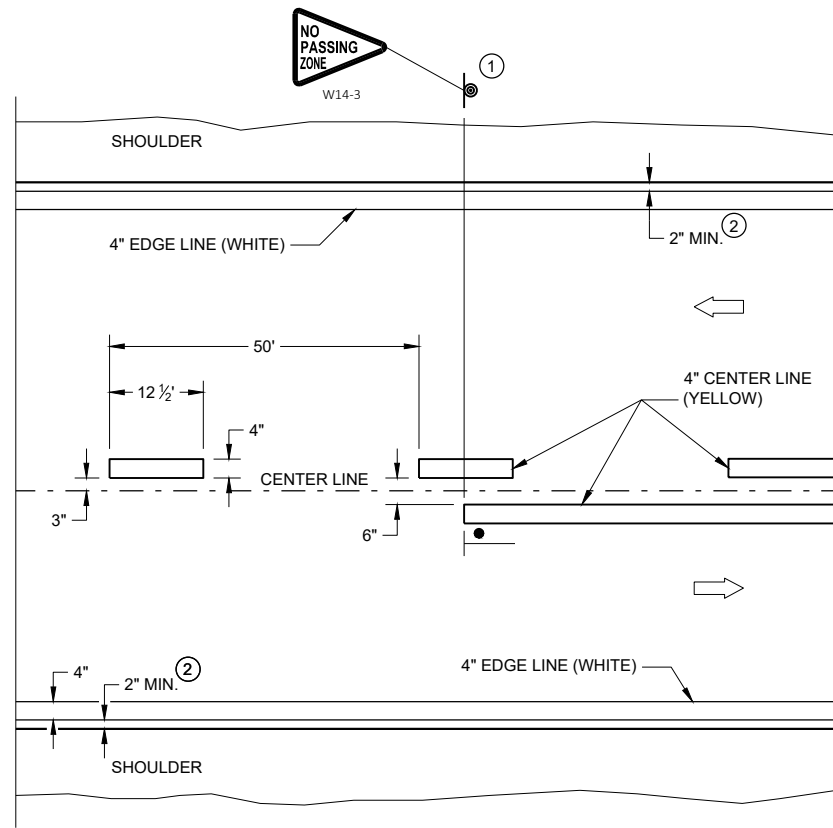
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING  
SIGNS 40 MPH OR LESS TWO-WAY  
UNDIVIDED ROAD OPEN TO TRAFFICE

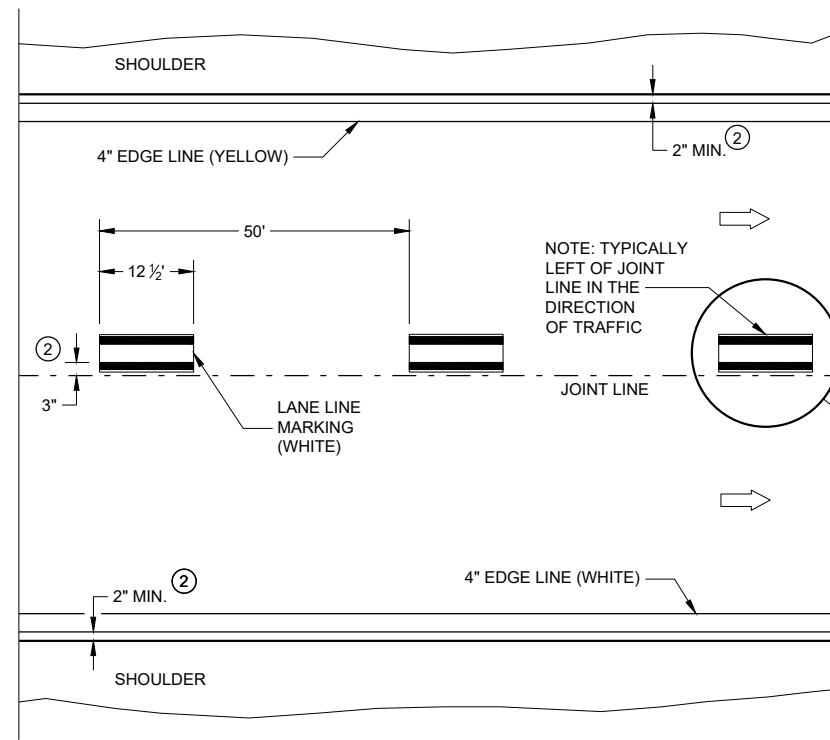
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

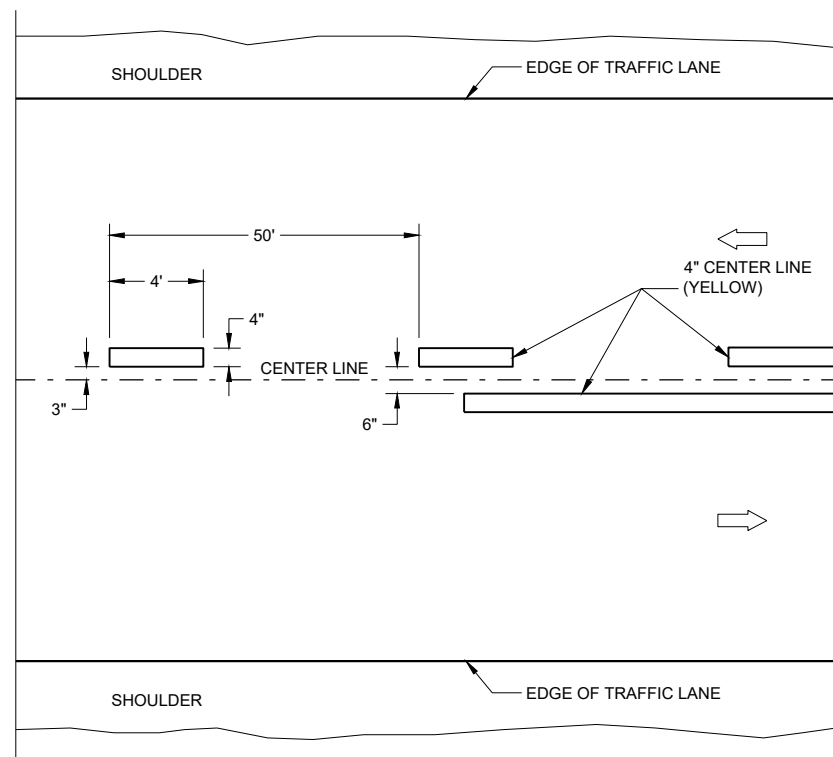


## TWO WAY TRAFFIC

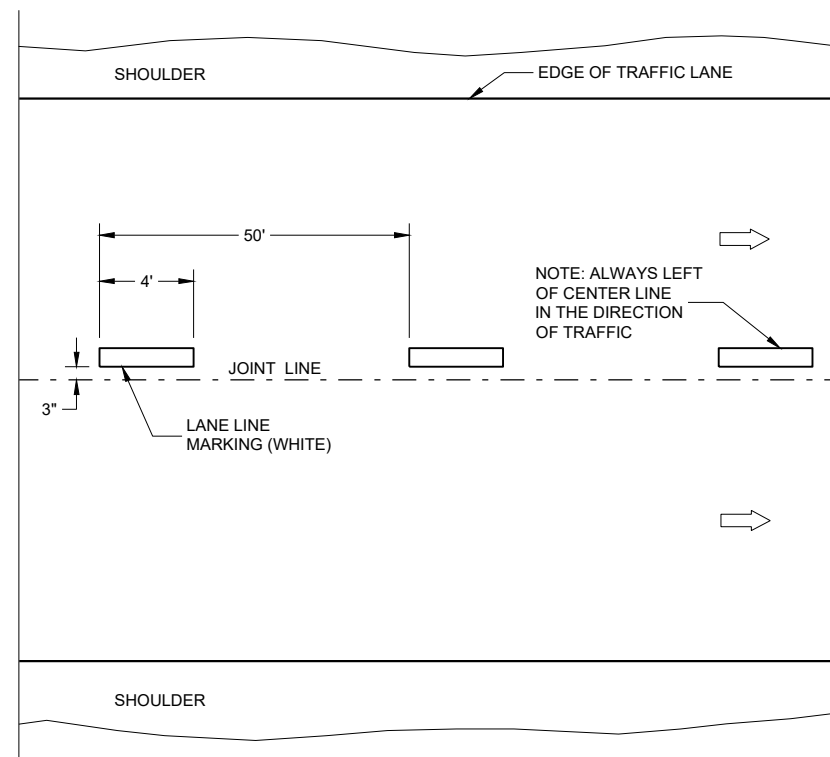


## ONE WAY TRAFFIC

## PERMANENT PAVEMENT MARKING



## TWO WAY TRAFFIC



## ONE WAY TRAFFIC




## TEMPORARY PAVEMENT MARKING

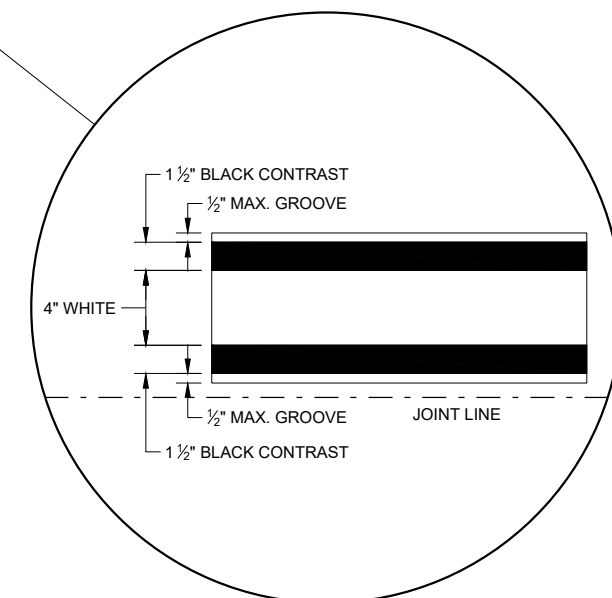
## GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITH 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

## LEGEND

-  "T" MARKING  
 SIGN ON PERMANENT SUPPORT  
 DIRECTION OF TRAFFIC








## LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020  
DATE

/S/ Matthew Rauch  
STATEWIDE SIGNING AND MARKING  
ENGINEER

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

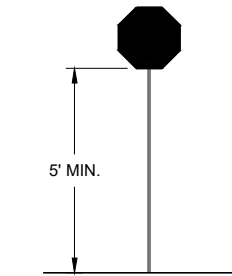
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



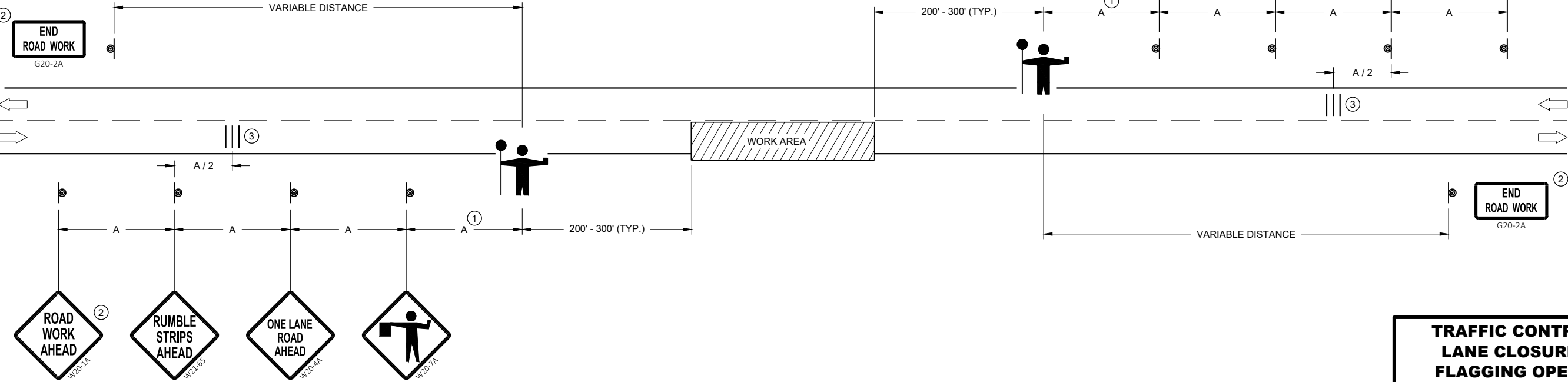
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'







USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

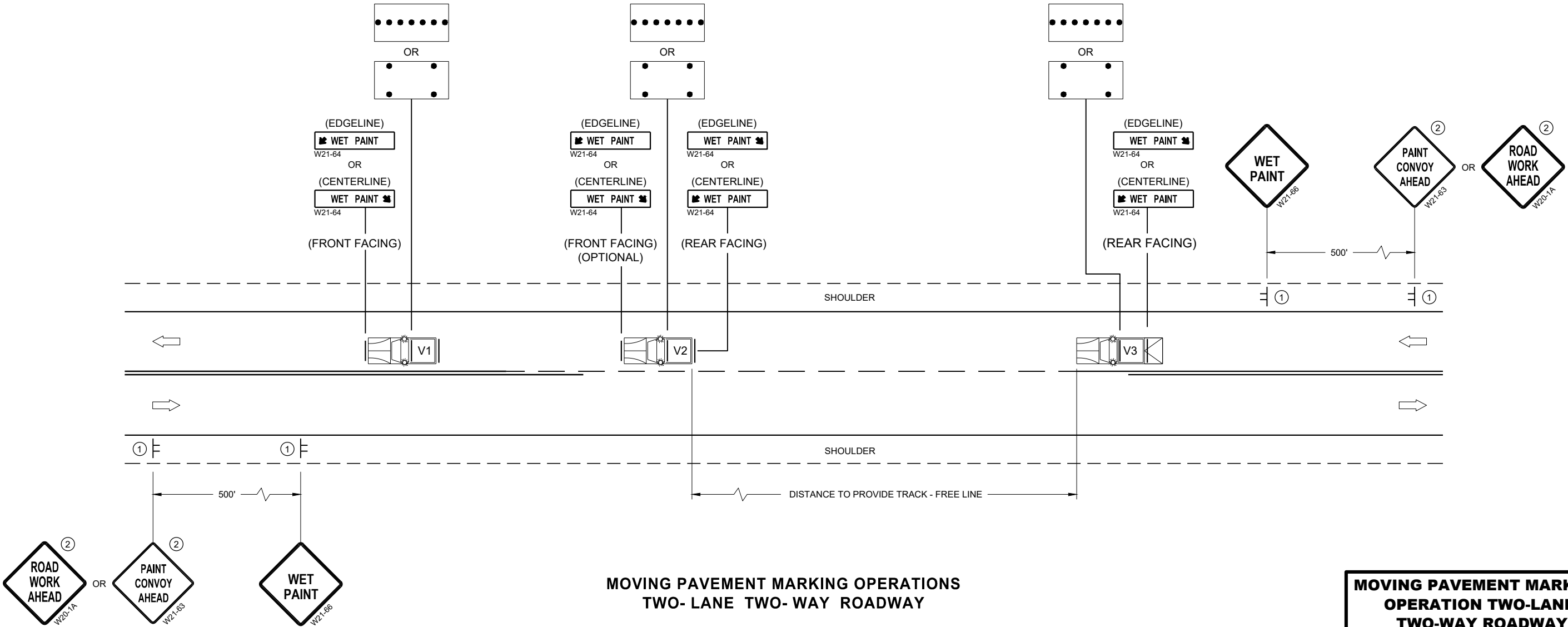
- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.
- DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

- WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.
- CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- CONES SHALL BE A MINIMUM OF 18" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.



MOVING PAVEMENT MARKING OPERATIONS  
TWO-LANE TWO-WAY ROADWAY

MOVING PAVEMENT MARKING  
OPERATION TWO-LANE  
TWO-WAY ROADWAY

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

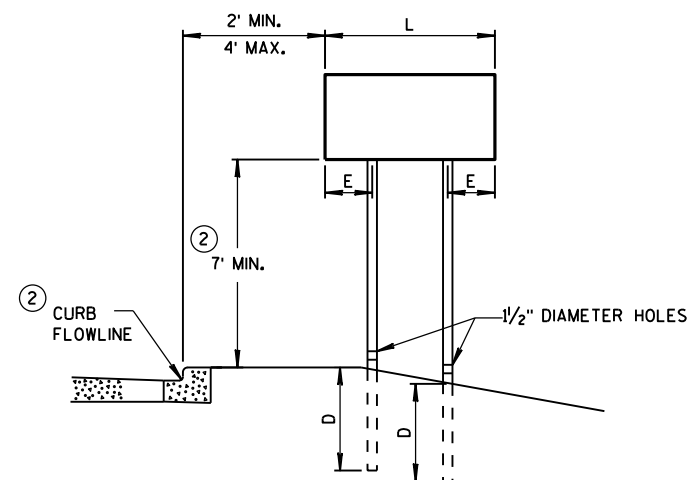
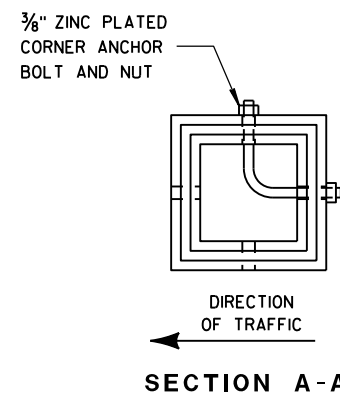
APPROVED  
November 2019 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA



## TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

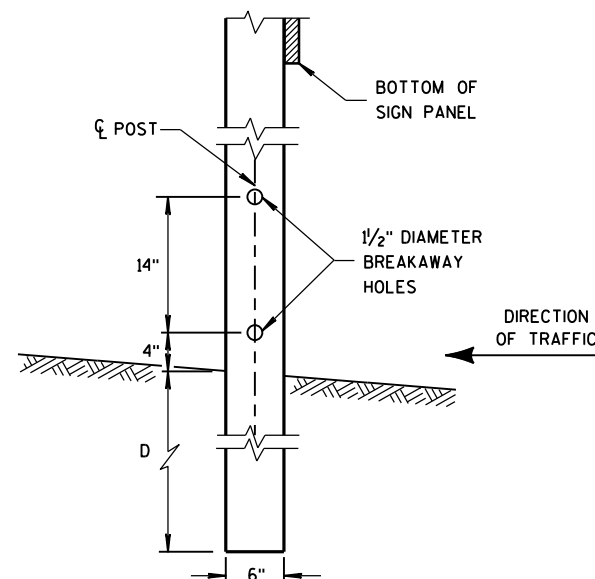


**URBAN AREA**

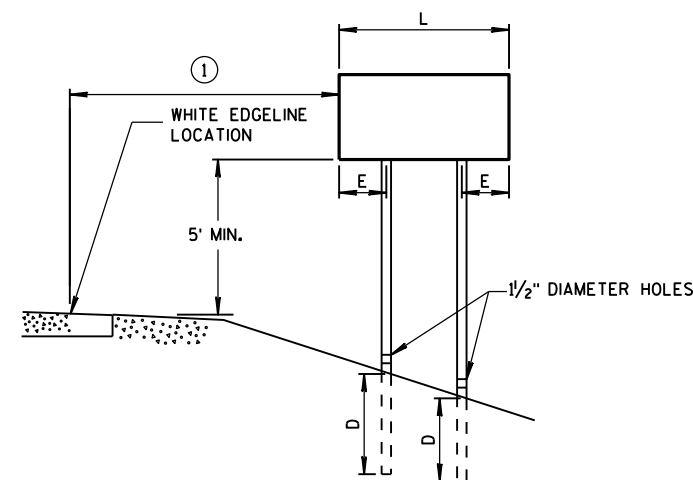
## POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST  
EMBEDMENT DEPTH

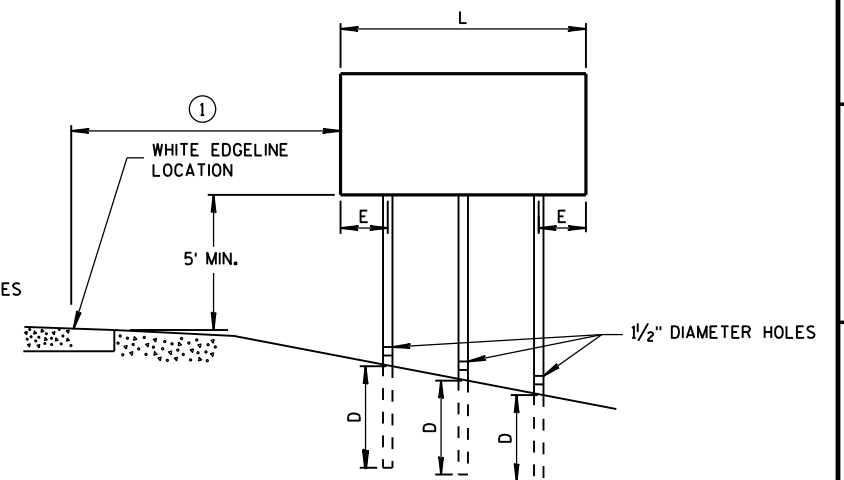
AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



## 4" x 6" WOOD POST MODIFICATION



## RURAL AREA



## GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

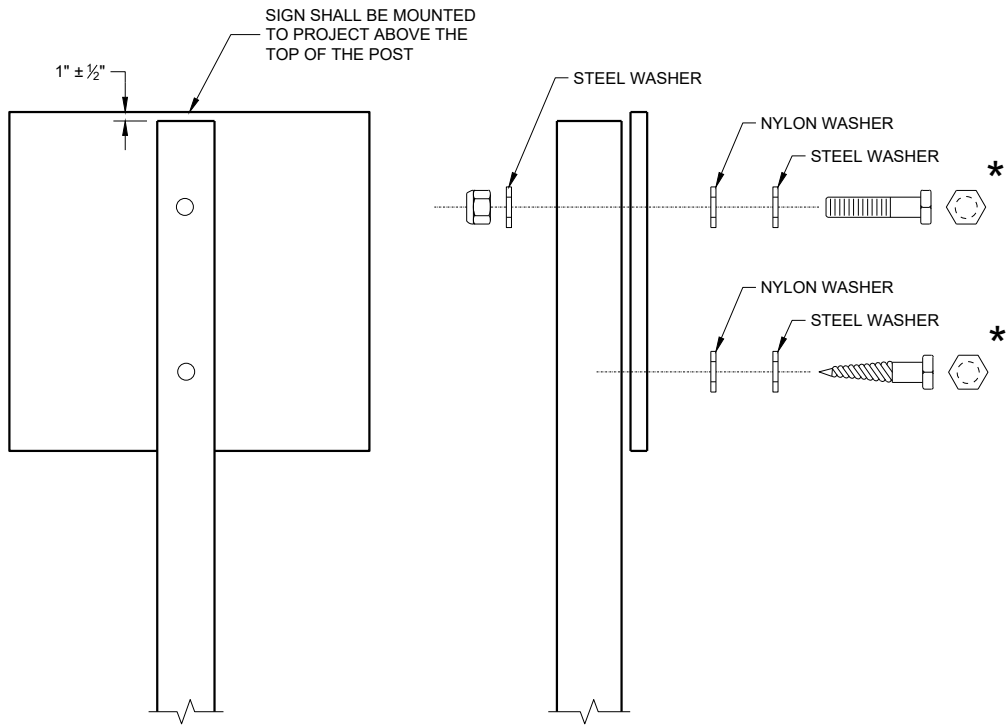
4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

SEE NOTE (3)

## TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS  
SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM  
DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM  
DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH  
SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED  
COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

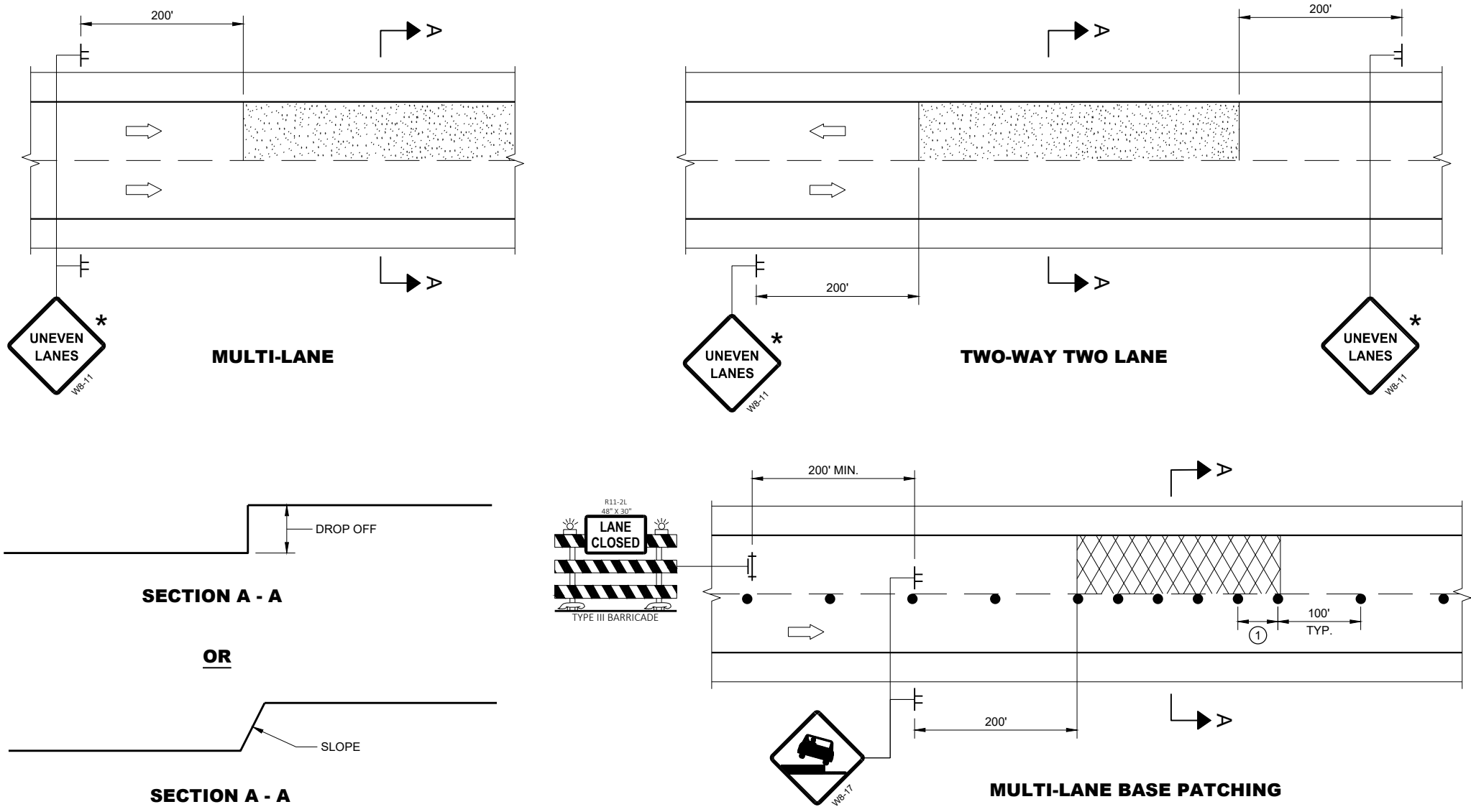
WOOD POST (4" x 6")  
LAG SCREWS - 3/8" x 3"  
MACHINE BOLTS - 5/16" x 6 1/2" OR 7" LENGTH W/NUTS

SQUARE STEEL POST (2" x 2")  
MACHINE BOLTS - 3/8" x 3 1/4" LENGTH W/NUTS  
RIVETS - 3/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM  
BODY/MANDREL O.D. FLANGE 0.720 - 0.765 INCH,  
GRIP RANGE 0.042 - 0.375 INCH

WASHERS (ALL POSTS) -  
1 1/4" O.D. x 3/8" I.D. x 1/16" STEEL  
1 1/4" O.D. x 3/8" I.D. x 0.080 NYLON

\* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION  
PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM  
SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH  
THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER  
THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



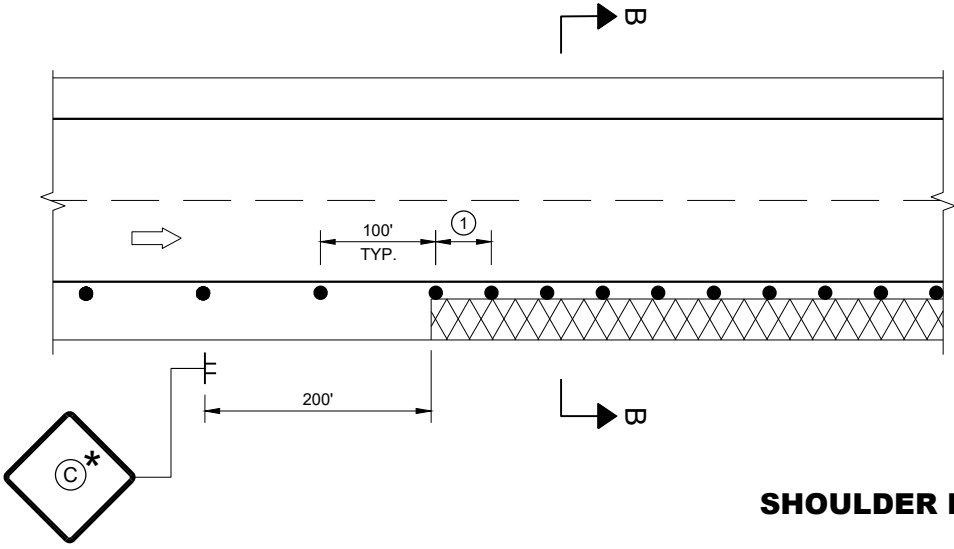
ADJACENT LANE DROP-OFFS

GENERAL NOTES

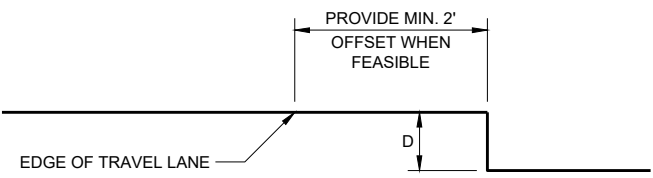
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- \* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN C
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

TRAFFIC CONTROL,  
DROP-OFF SIGNING

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
March 2018  
DATE

/S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

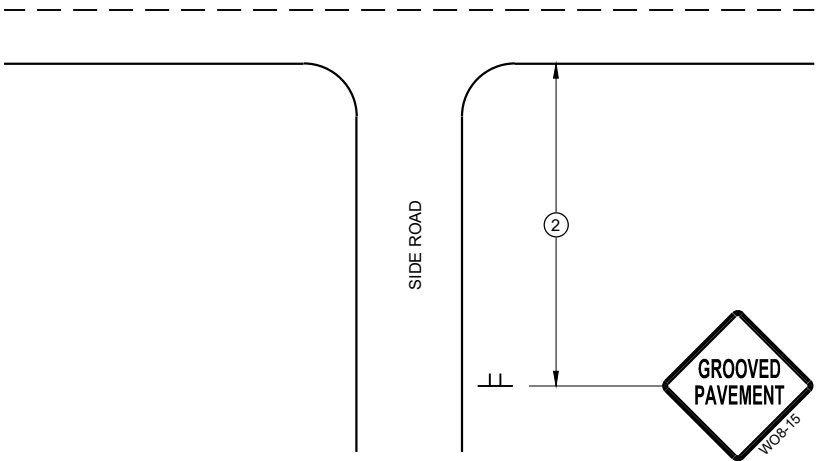
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

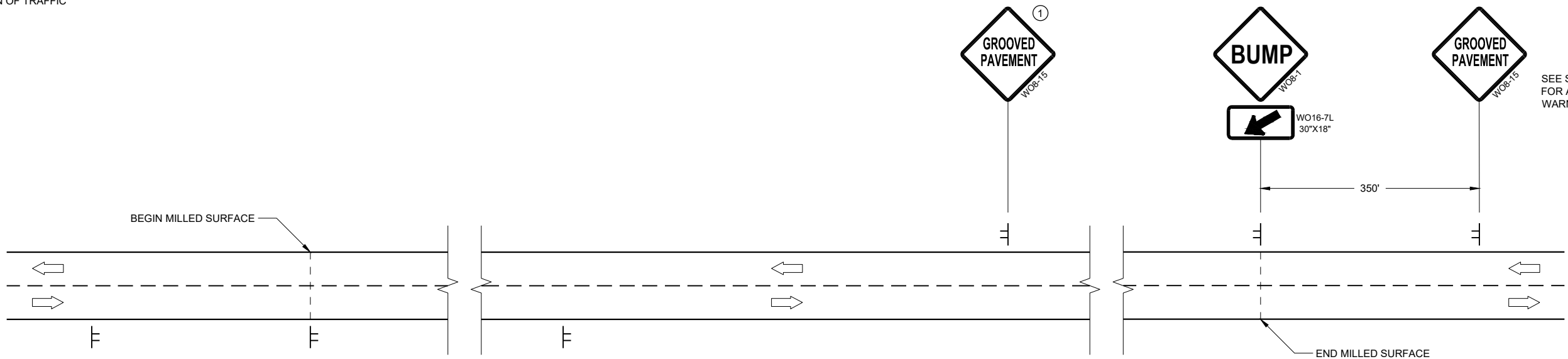
- 1 PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- 2 PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH  
SIGN DETAIL



SEE SDD15C04  
FOR ADVANCE  
WARNING SIGNS

SEE SDD15C04  
FOR ADVANCE  
WARNING SIGNS

DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL,  
SIGNING ON ROADWAYS  
WITH MILLED SURFACES

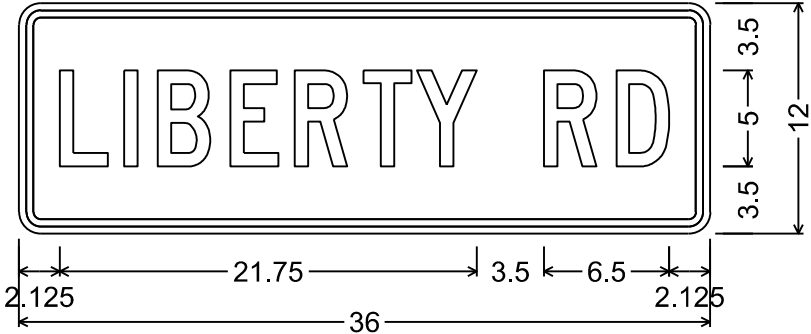
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA



- NOTES:
- 1. SIGN IS TYPE II - TYPE F REFLECTIVE - REFERENCE "WISDOT STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION" LATEST EDITION.
  - 2. COLOR  
BACKGROUND - ORANGE  
MESSAGE - BLACK
  - 3. MESSAGE SERIES - C
  - 4. CORNERS MAY BE SQUARE OR ROUNDED WHEN BASE MATERIAL IS PLYWOOD, BUT BORDERS SHALL BE ROUNDED AS SHOWN.  
WHEN BASE MATERIAL IS METAL, THE CORNERS SHALL BE ROUNDED.



1.125" Radius, 0.375" Border, 0.375" Indent, Black on, Orange;

7

7



## ***Wisconsin Department of Transportation***

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through innovation and exceptional service.

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