

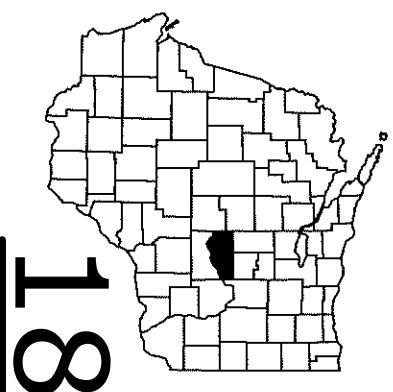
WIS  
PROJECT ID: 6140-01-65  
COUNTY: ADAMS

JUNE 2021

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 78



DESIGN DESIGNATION

A.A.D.T.	2018	=	5810
A.A.D.T.		=	
D.H.V.		=	
D.D.		=	
T.		=	
DESIGN SPEED		=	
ESALS		=	

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

ADAMS - WISCONSIN RAPIDS

ADAMS BRIDGES B-1-17,36

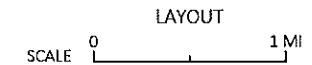
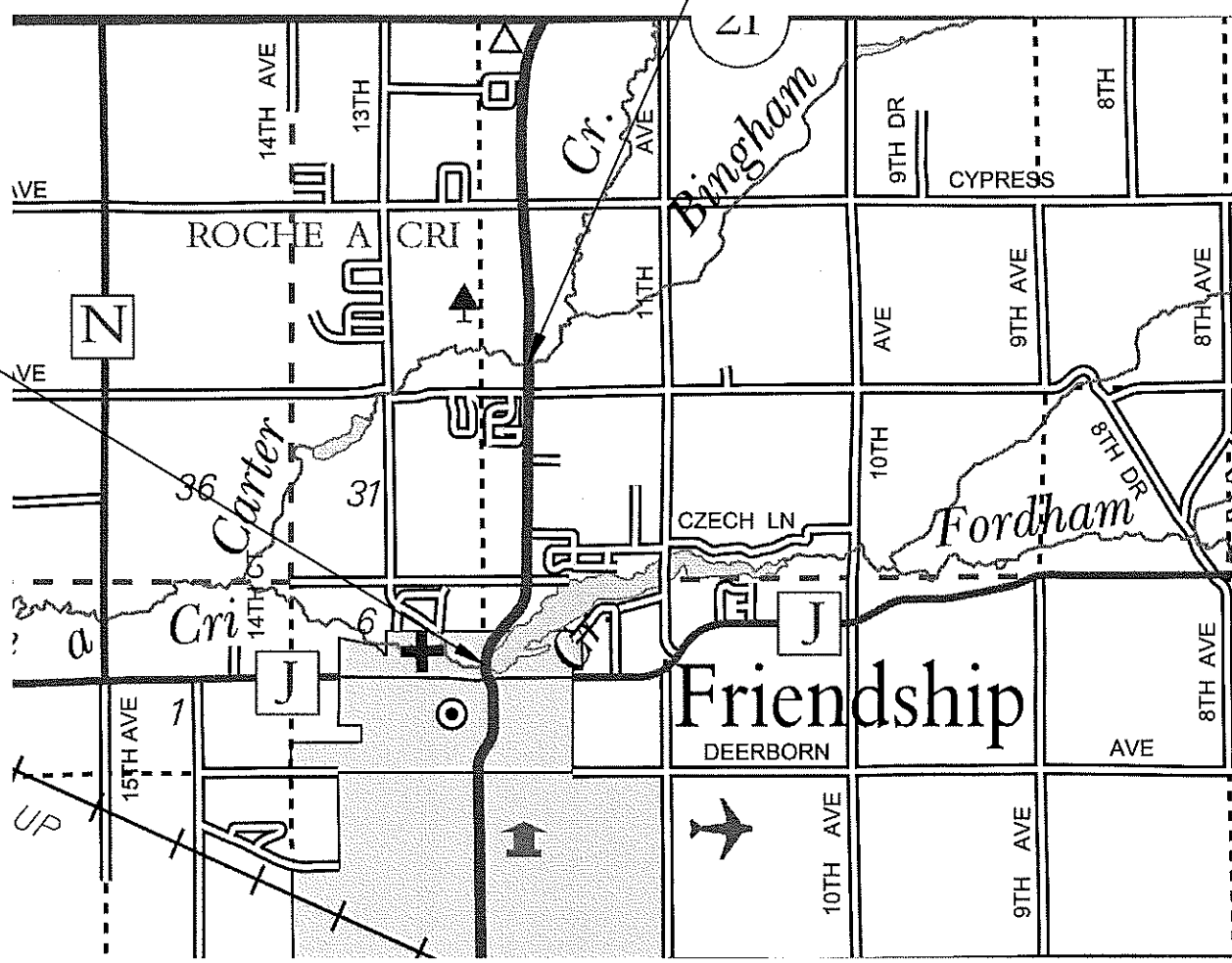
STH 13  
ADAMS

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6140-01-65	WISC 2021410	1

STATE PROJECT NUMBER
6140-01-65

STRUCTURE B-1-36  
STH 13 OVER CARTER CREEK

STRUCTURE B-1-17  
STH 13 OVER LITTLE ROCHE A CRI RIVER



TOTAL NET LENGTH OF CENTERLINE =

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), ADAMS COUNTY, NAD83 ( 2011 ), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 ( 2012 ), GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
PREPARED BY	
Surveyor	NC REGION
Designer	JUSTIN WIGAND
Project Manager	JEFFREY STEWART
Regional Examiner	CHERYL SIMON
Regional Supervisor	NICHOLE LYSNE
APPROVED FOR THE DEPARTMENT	
DATE: 10/25/18	

GENERAL NOTES

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

TOPSOIL SHALL BE PLACED 1" BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

Runoff Coefficient Table

	Hydrologic Soil Group											
	A			B			C			D		
	Slope Range (Percent)											
Land Use	0 - 2	2 - 6	6 & Over	0 - 2	2 - 6	6 & Over	0 - 2	2 - 6	6 & Over	0 - 2	2 - 6	6 & Over
Row Crops	0.08	0.16	0.22	0.12	0.20	0.27	0.15	0.24	0.33	0.19	0.28	0.38
	0.22	0.30	0.38	0.26	0.34	0.44	0.30	0.37	0.50	0.34	0.41	0.56
Median Strip-Turf	0.19	0.20	0.24	0.19	0.22	0.26	0.20	0.23	0.30	0.20	0.25	0.30
	0.24	0.26	0.30	0.25	0.28	0.33	0.26	0.30	0.37	0.27	0.32	0.40
Side Slope Turf			0.25			0.27			0.28			0.30
			0.32			0.34			0.36			0.38
Pavement												
Asphalt	0.70 - 0.95											
Concrete	0.80 - 0.95											
Brick	0.70 - 0.80											
Drives, Sidewalks	0.75 - 0.85											
Roofs	0.75 - 0.95											
Gravel Roads, Shoulders	0.40 - 0.60											

Total Project Area = 1 Acres  
Total Area Expected To Be Disturbed By Construction Activities = 0.025 Acres

UTILITIES

FRONTIER COMMUNICATIONS - COMMUNICATION LINE

JENNY MOORE  
2222 WEST WISCONSIN STREET  
PORTAGE, WI 53901  
PHONE: (608) 742-9507 WORK  
PHONE: (608) 346-0353 MOBILE  
E-MAIL: JERALD.R.MOORE@FTR.COM

ALLIANT ENERGY - ELECTRICITY

PETER FRITZ  
338 EAST STATE STREET  
MAUTSON, WI 53948  
PHONE: (608) 847-1317 WORK  
PHONE: (608) 963-5676 MOBILE  
E-MAIL: PETERFRITZ@ALLIANTENERGY.COM

ATC MANAGEMENT - ELECTRICITY

CHRIS DAILEY  
W 234 N 2000 RIDGEVIEW PARKWAY CT  
PO BOX 47  
WAUKESHA, WI 53187  
PHONE: (262) 506-6884 WORK  
E-MAIL: CDAILEY@ATCLLC.COM

DNR

BRAD BETTHAUSER

473 GRIFFITH AVENUE  
WISCONSIN RAPIDS, WI 54494  
PHONE: (715) 421-7851  
E-MAIL: BRADLEY.BETTHAUSER@WISCONSIN.GOV

WE ENERGIES - GAS/PETROLEUM

RYAN MIENTKE  
1921 8TH STREET SOUTH  
WISCONSIN RAPIDS, WI 54494  
PHONE: (715) 421-7249 WORK  
PHONE:(715) 421-9293 MOBILE  
E-MAIL: RYAN.MIENTKE@WE-ENERGIES.COM

VILLAGE OF FRIENDSHIP - SEWER

BOB KIERSTYN  
507 WEST LAKE STREET  
PO BOX 206  
FRIENDSHIP, WI 53934  
PHONE: (608) 339-3243 WORK  
PHONE: (608) 548-8575 MOBILE  
E-MAIL: VOFPUBLICWORKS@MAQS.NET

VILLAGE OF FRIENDSHIP - WATER

BOB KIERSTYN  
507 WEST LAKE STREET  
PO BOX 206  
FRIENDSHIP, WI 53934  
PHONE: (608) 339-3243 WORK  
PHONE: (608) 548-8575  
E-MAIL: VOFPUBLICWORKS@MAQS.NET



Dial 811 or (800)242-8511  
www.DiggersHotline.com

STRUCTURE B-1-17  
LITTLE ROCHE A CRI CREEK



PROJECT NO	6140-01-65
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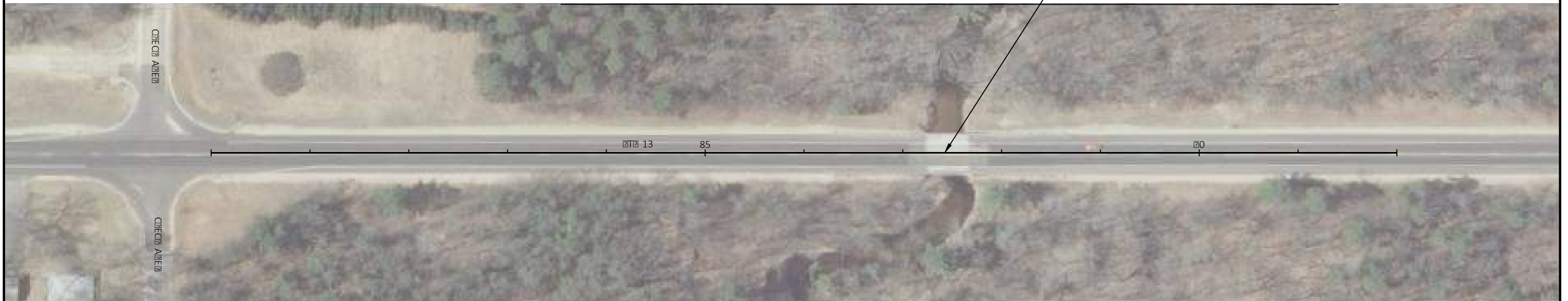
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CO??TY? ADAM?

## PROJECT OVERVIEW

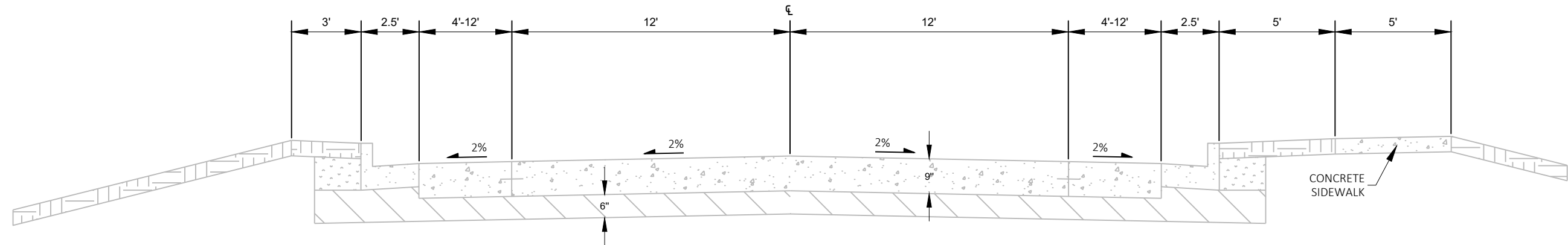
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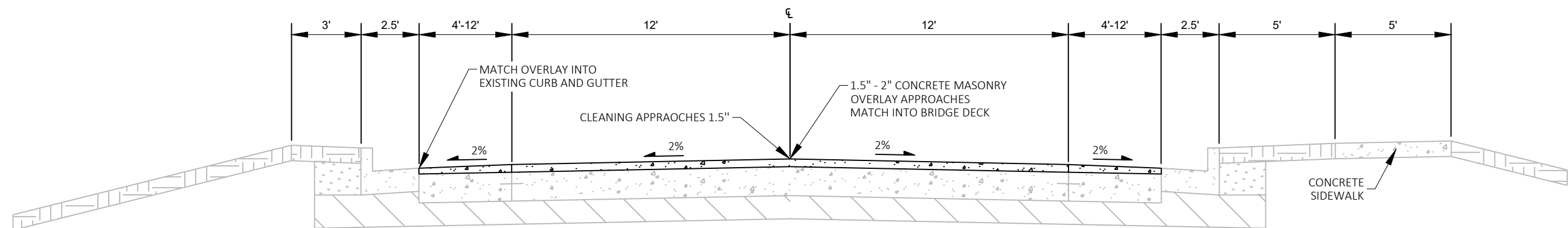
STRUCTURE B-1-36  
CARTER CREEK





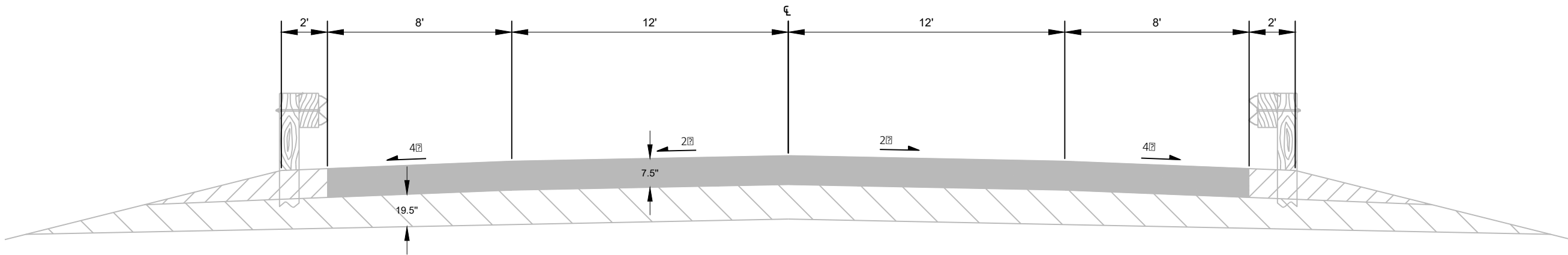
### EXISTING TYPICAL SECTION

STA 18+11 - STA 18+51  
STA 18+98 - STA 19+43



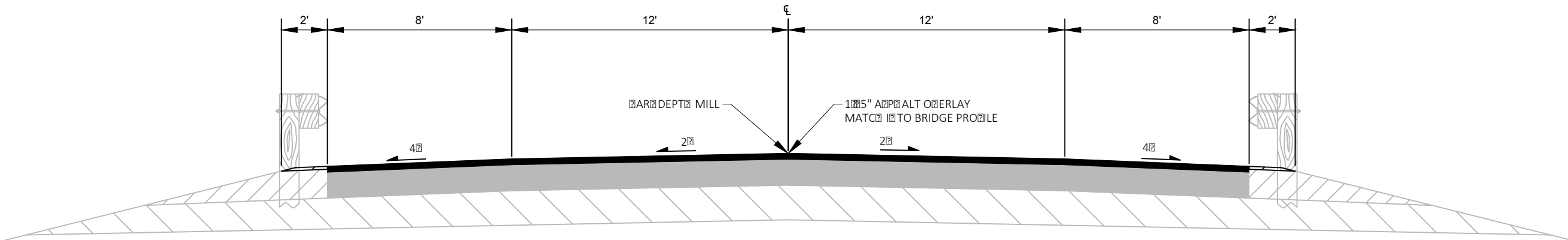
### PROPOSED TYPICAL SECTION

STA 18+11 - STA 18+51  
STA 18+98 - STA 19+43



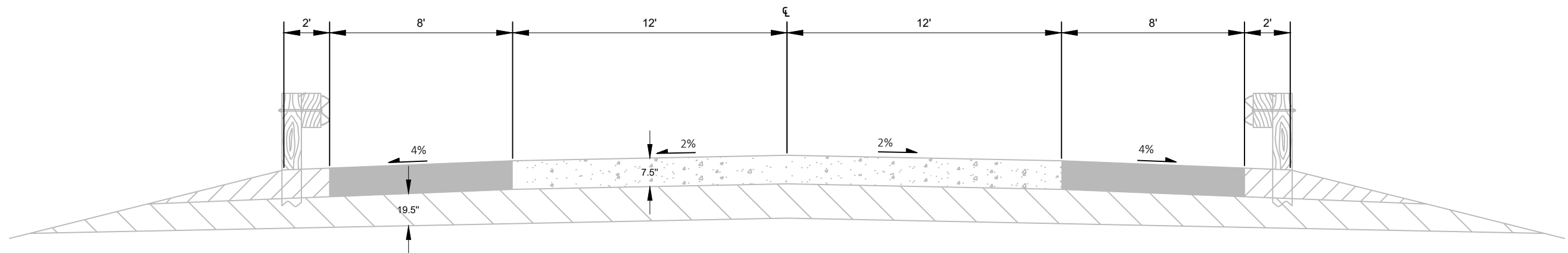
EXISTING TYPICAL SECTION

STA 86+24 - STA 87+04  
STA 87+86 - STA 88+66



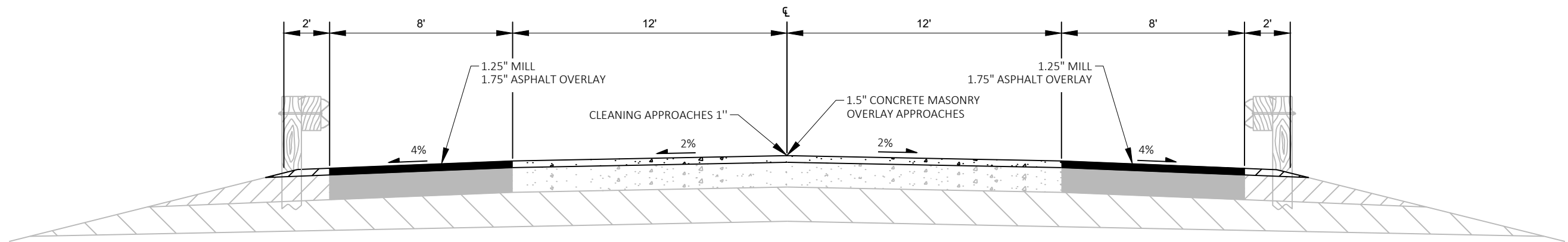
PROPOSED TYPICAL SECTION

STA 86+24 - STA 87+04  
STA 87+86 - STA 88+66



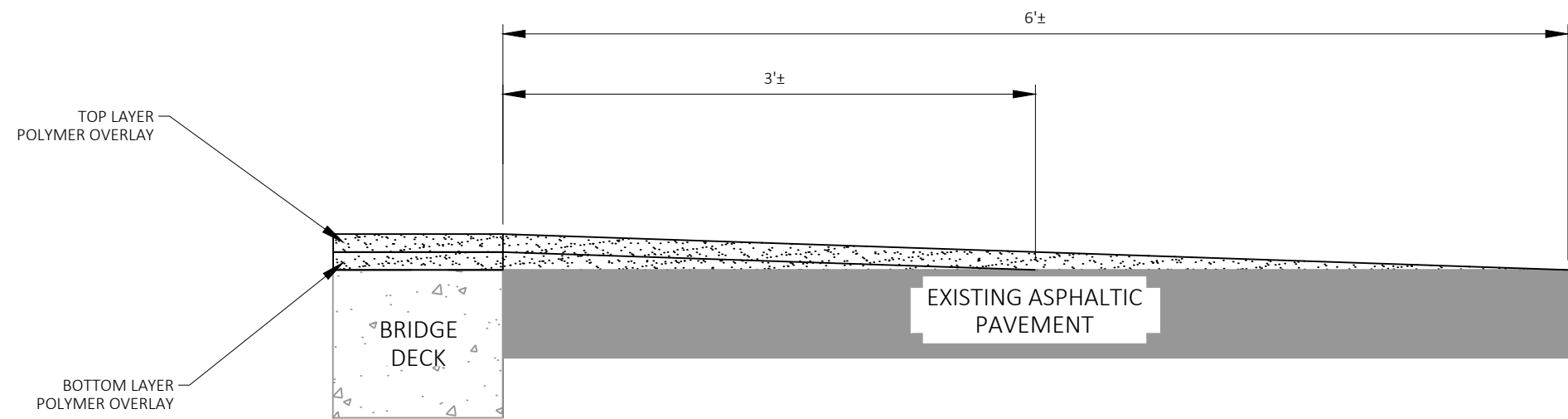
### EXISTING TYPICAL SECTION

STA 87+04 - STA 87+24  
STA 87+66 - STA 87+86



### PROPOSED TYPICAL SECTION

STA 87+04 - STA 87+86

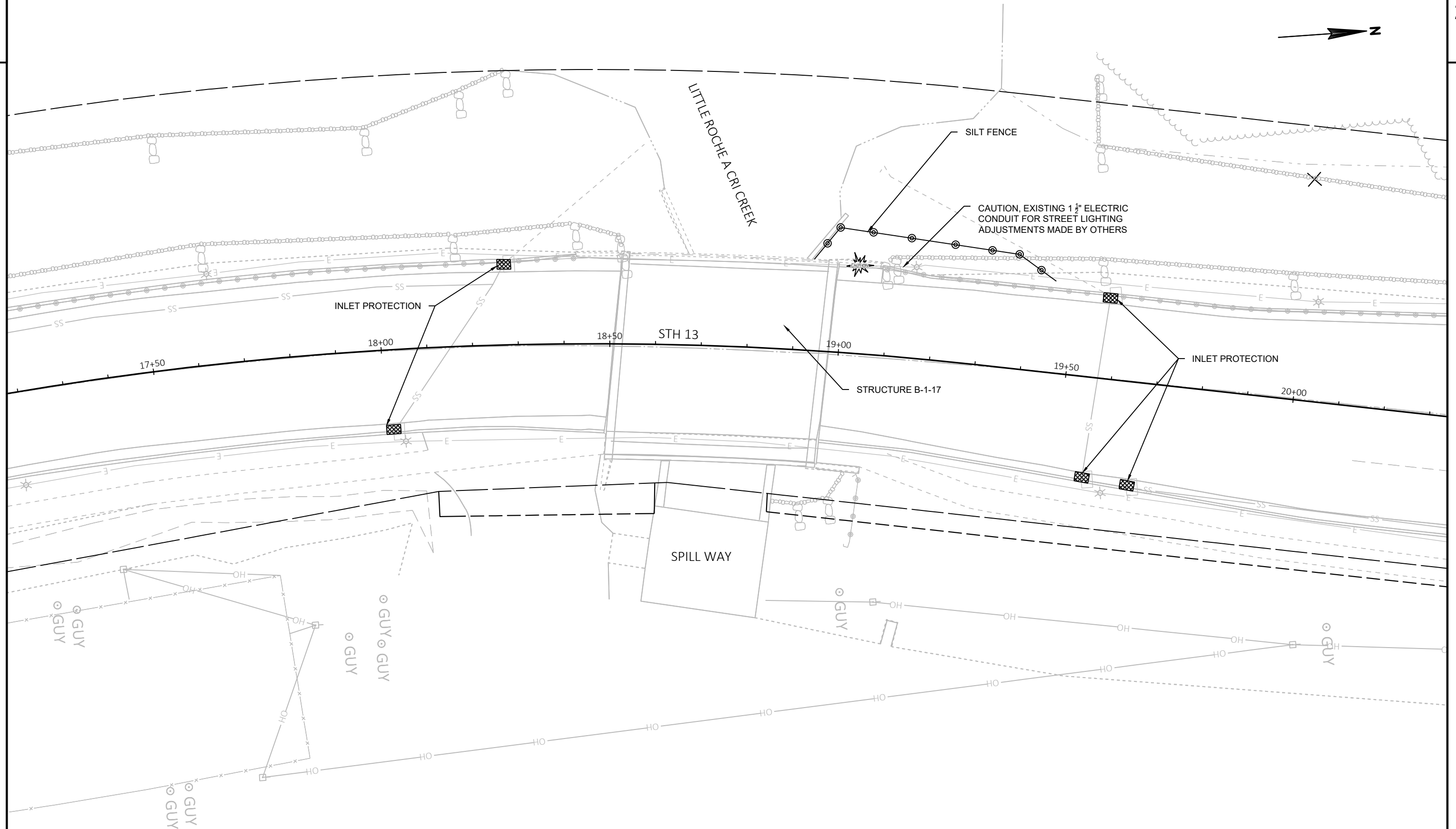


TRANSITION AT STRUCTURES

STA 82+18.40 - STA 82+24.40  
STA 83+45.94 - STA 83+51.94  
STA 104+83.23 - STA 104+89.23  
STA 105+88.77 - STA 105+94.77

Notes:

1. POLYMER OVERLAY TO BE PLACED FULL WIDTH COVERING PAVEMENT AND SHOULDERS
2. MASK OFF EXISTING END JOINT AS DIRECTED BY ENGINEER



PROJECT NO: 6140-01-65	HWY: STH 13	COUNTY: ADAMS	EROSION CONTROL - B-01-17	SHEET	E
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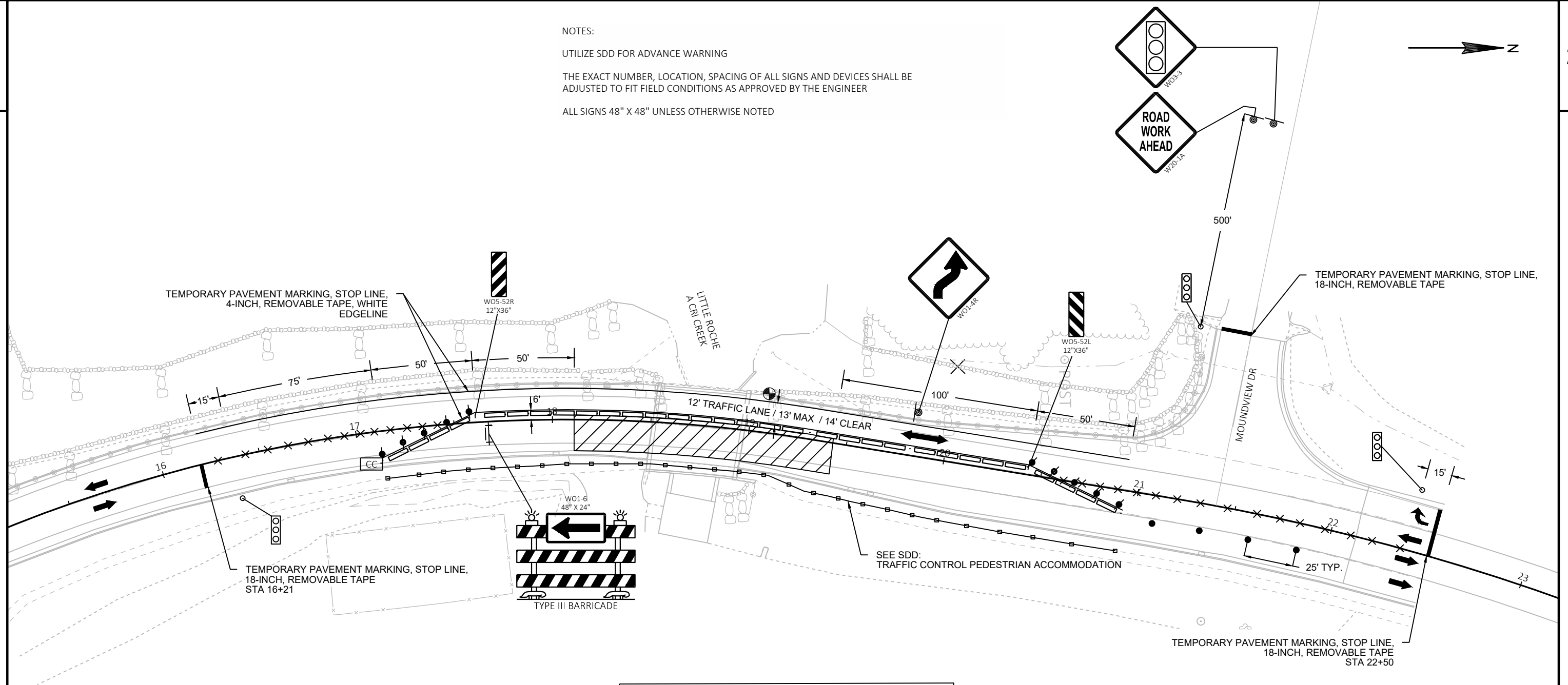


## NOTES:

UTILIZE SDD FOR ADVANCE WARNING

THE EXACT NUMBER, LOCATION, SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER

ALL SIGNS 48" X 48" UNLESS OTHERWISE NOTED



## LEGEND

CC

PORTABLE CRASH CUSHION

+

TYPE III BARRICADE WITH ATTACHED SIGN

●

TRAFFIC CONTROL DRUM

●

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

⊙

SIGN ON TEMPORARY SUPPORT

→

DIRECTION OF TRAFFIC

⊙

TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)

▨

WORK AREA

XXXX

REMOVE PAVEMENT MARKING

□

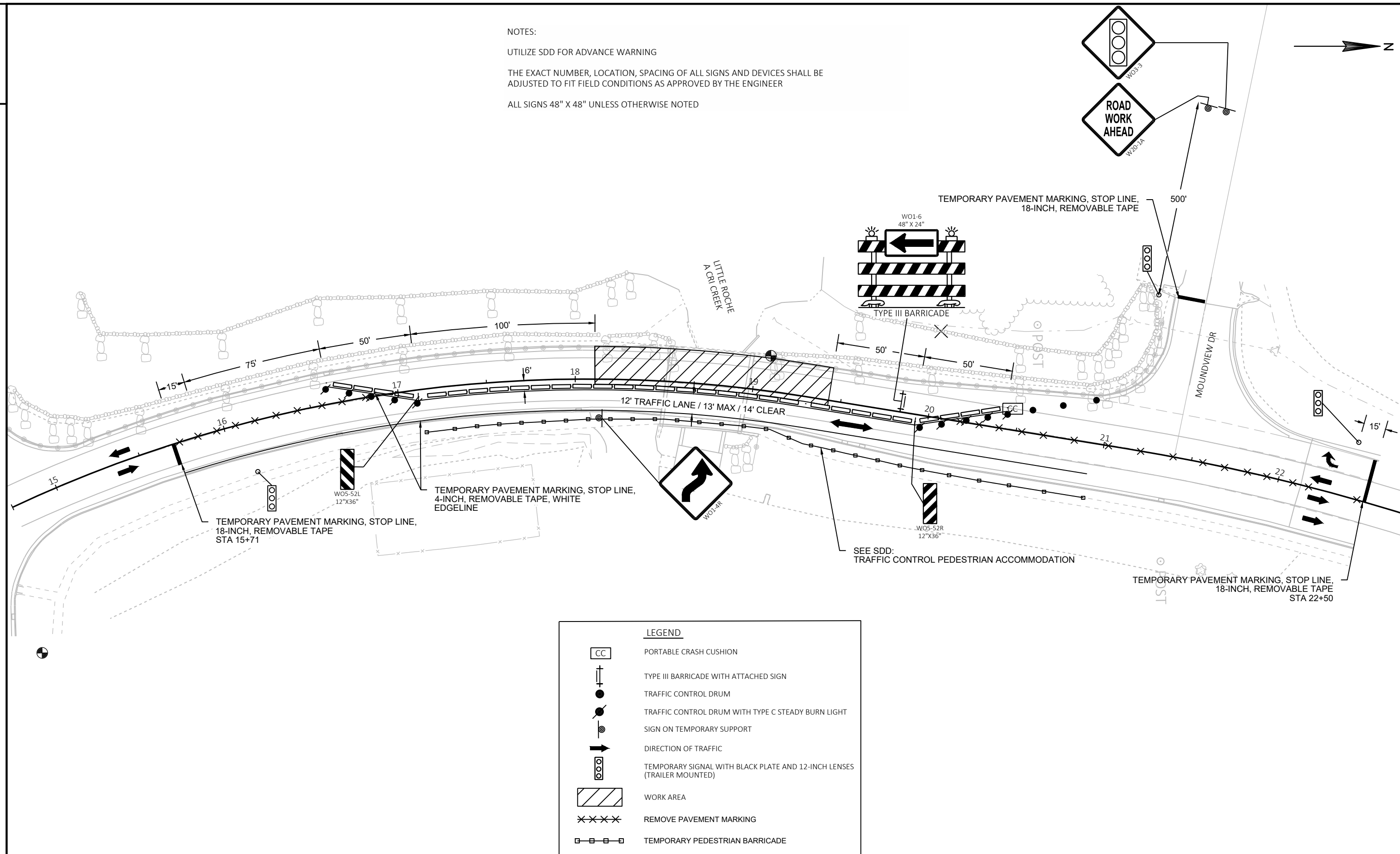
TEMPORARY PEDESTRIAN BARRICADE

## NOTES:

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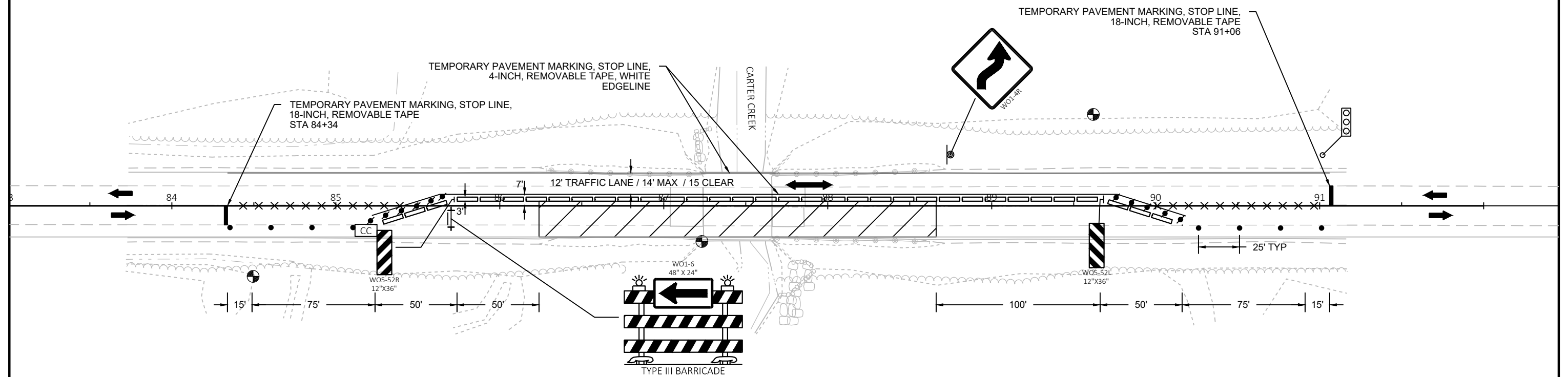


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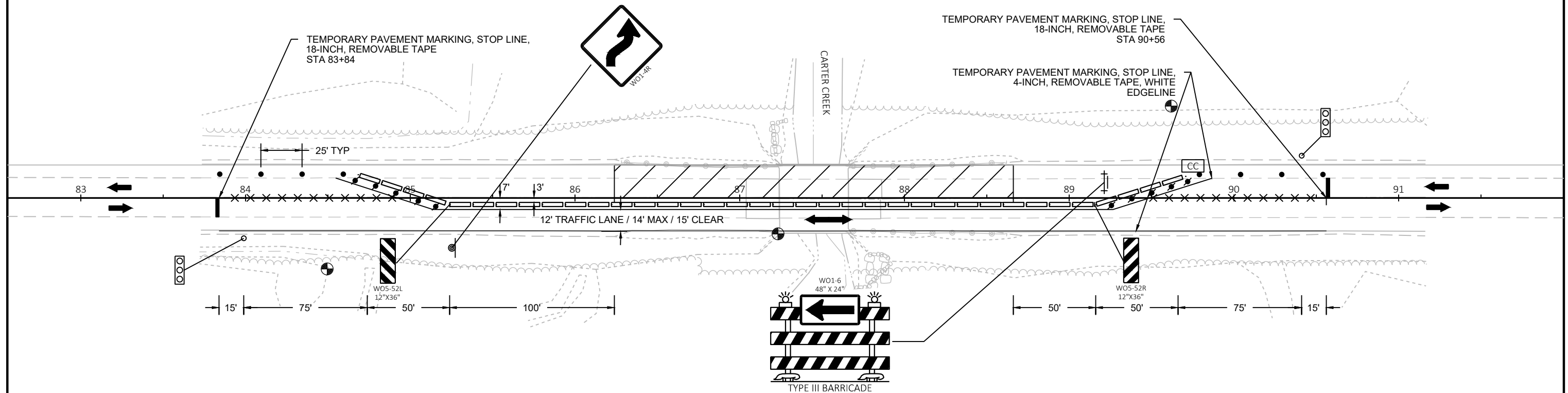
CC	PORTABLE CRASH CUSHION
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON TEMPORARY SUPPORT
	DIRECTION OF TRAFFIC
	TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
	WORK AREA
	REMOVE PAVEMENT MARKING
	TEMPORARY PEDESTRIAN BARRICADE

## NOTES:

UTILIZE SDD FOR ADVANCE WARNING

THE EXACT NUMBER, LOCATION, SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER

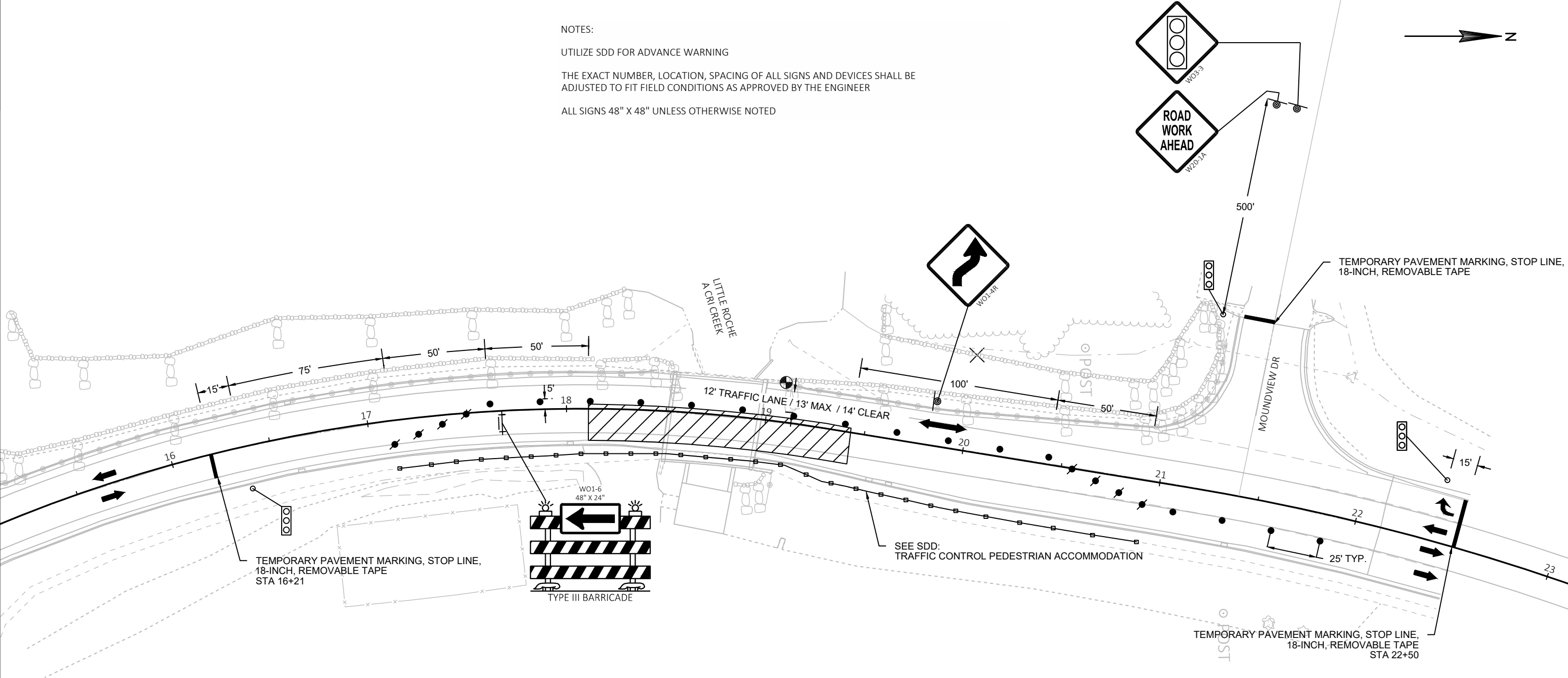
ALL SIGNS 48" X 48" UNLESS OTHERWISE NOTED



## LEGEND

	PORTABLE CRASH CUSHION
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON TEMPORARY SUPPORT
	DIRECTION OF TRAFFIC
	TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
	WORK AREA
	REMOVE PAVEMENT MARKING
	TEMPORARY PEDESTRIAN BARRICADE

NOTES:  
UTILIZE SDD FOR ADVANCE WARNING  
THE EXACT NUMBER, LOCATION, SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER  
ALL SIGNS 48" X 48" UNLESS OTHERWISE NOTED



LEGEND

CC

PORTABLE CRASH CUSHION

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

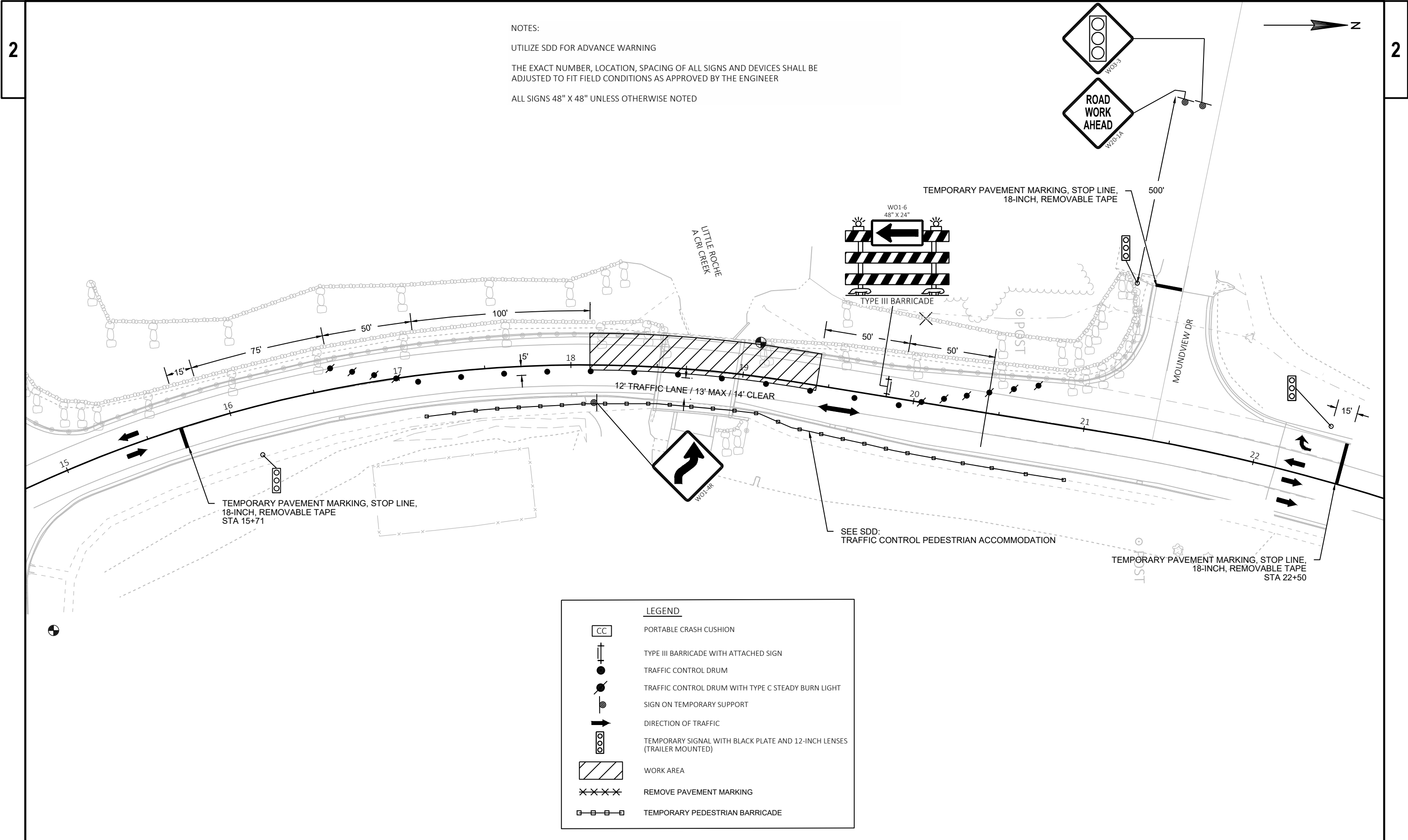
TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)

WORK AREA

REMOVE PAVEMENT MARKING

TEMPORARY PEDESTRIAN BARRICADE





NOTES:

UTILIZE SDD FOR ADVANCE WARNING

THE EXACT NUMBER, LOCATION, SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER

ALL SIGNS 48" X 48" UNLESS OTHERWISE NOTED

LEGEND

CC

TYPE III BARRICADE WITH ATTACHED SIGN

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PORTABLE CRASH CUSHION

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)

WORK AREA

REMOVE PAVEMENT MARKING

TEMPORARY PEDESTRIAN BARRICADE

PROJECT NO: 6140-01-65

HWY: STH 13

COUNTY: ADAMS

TRAFFIC CONTROL B-1-17 STAGE 4

SHEET

E

FILE NAME : C:\ONEDRIVE - STATE OF WISCONSIN\DESKTOP\REFERENCE PROJECTS\61400135\SHEETS\PLAN\025001-TC.DWG

PLOT DATE : 4/6/2021 11:41 AM

PLOT BY : NEMEC, ELIZABETH ANN

PLOT NAME :

PLOT SCALE : 1 IN:50 FT

LAYOUT NAME - 06

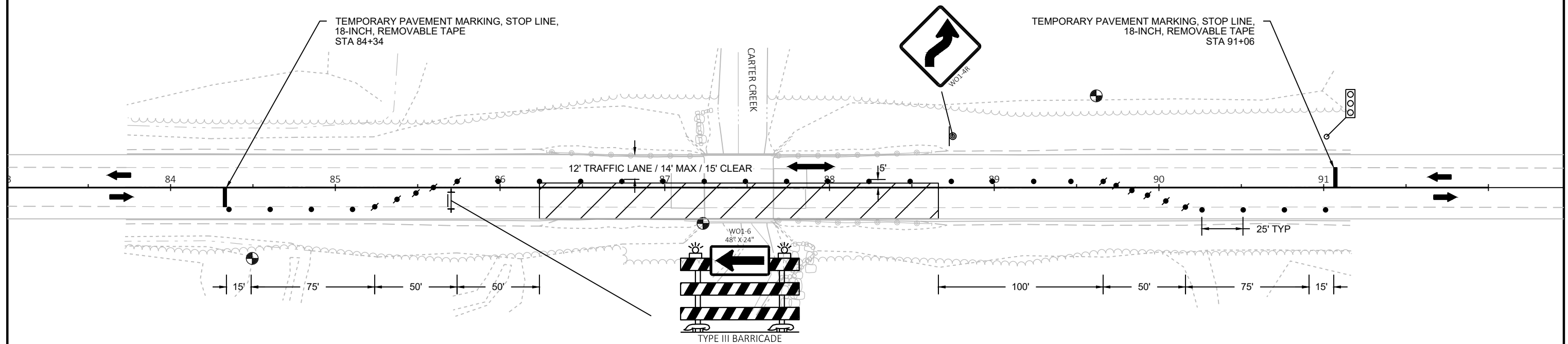
WISDOT/CADDs SHEET 44

## NOTES:

UTILIZE SDD FOR ADVANCE WARNING

THE EXACT NUMBER, LOCATION, SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER

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## LEGEND

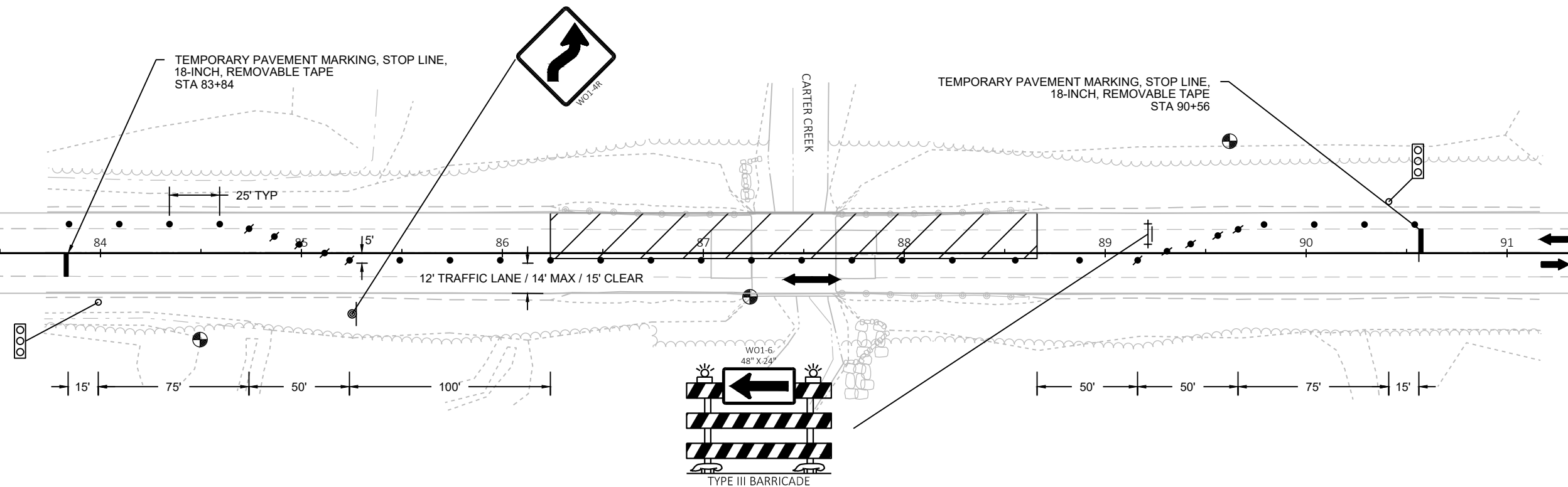
	PORTABLE CRASH CUSHION
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
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	WORK AREA
	REMOVE PAVEMENT MARKING
	TEMPORARY PEDESTRIAN BARRICADE

## NOTES:

UTILIZE SDD FOR ADVANCE WARNING

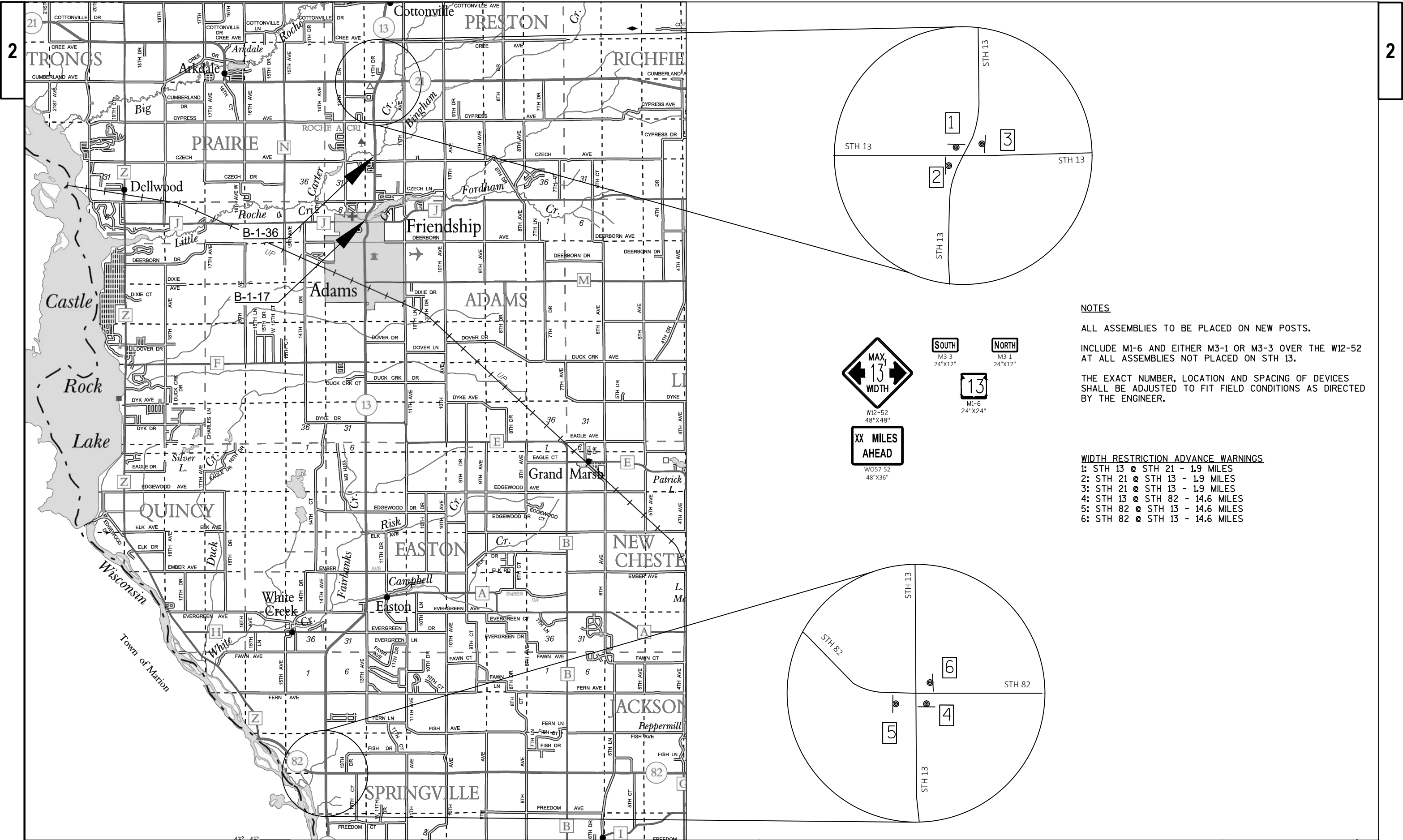
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## LEGEND

	PORTABLE CRASH CUSHION
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON TEMPORARY SUPPORT
	DIRECTION OF TRAFFIC
	TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
	WORK AREA
	REMOVE PAVEMENT MARKING
	TEMPORARY PEDESTRIAN BARRICADE



**NOTES**

ALL ASSEMBLIES TO BE PLACED ON NEW POSTS.

INCLUDE M1-6 AND EITHER M3-1 OR M3-3 OVER THE W12-52 AT ALL ASSEMBLIES NOT PLACED ON STH 13.

THE EXACT NUMBER, LOCATION AND SPACING OF DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

**WIDTH RESTRICTION ADVANCE WARNINGS**

- 1: STH 13 @ STH 21 - 1.9 MILES
- 2: STH 21 @ STH 13 - 1.9 MILES
- 3: STH 21 @ STH 13 - 1.9 MILES
- 4: STH 13 @ STH 82 - 14.6 MILES
- 5: STH 82 @ STH 13 - 14.6 MILES
- 6: STH 82 @ STH 13 - 14.6 MILES

PROJECT NO: 6140-01-65

HWY: STH 13

COUNTY: ADAMS

TRAFFIC CONTROL - WIDTH RESTICTION ADVANCED WARNING

SHEET

E

**TEMPORARY SIGNAL TIMING B-1-17**  
SIGNAL TIMING PROVIDED BY NC REGION TRAFFIC SECTION

STAGE 1 AND STAGE 3		STAGE 2 AND STAGE 4	
6:00 AM TO 8:00 PM	8:00 PM TO 6:00 AM	6:00 AM TO 8:00 PM	8:00 PM TO 6:00 AM
NB STH 13	NB STH 13	NB STH 13	NB STH 13
GREEN 29 SEC	GREEN 24 SEC	GREEN 30 SEC	GREEN 24 SEC
YELLOW 3 SEC	YELLOW 3 SEC	YELLOW 3 SEC	YELLOW 3 SEC
RED 21 SEC	RED 21 SEC	RED 22 SEC	RED 22 SEC
SB STH 13	SB STH 13	SB STH 13	SB STH 13
GREEN 29 SEC	GREEN 24 SEC	GREEN 30 SEC	GREEN 24 SEC
YELLOW 3 SEC	YELLOW 3 SEC	YELLOW 3 SEC	YELLOW 3 SEC
RED 6 SEC	RED 6 SEC	RED 6 SEC	RED 6 SEC
EB MOUNDVIEW DR	EB MOUNDVIEW DR	EB MOUNDVIEW DR	EB MOUNDVIEW DR
GREEN 5 SEC	GREEN 4 SEC	GREEN 5 SEC	GREEN 4 SEC
YELLOW 3 SEC	YELLOW 6 SEC	YELLOW 3 SEC	YELLOW 6 SEC
RED 20 SEC	RED 20 SEC	RED 21 SEC	RED 21 SEC

PHASES SHALL GO IN ORDER AS PRESENTED. PHASES SHALL NOT BE SKIPPED. SIGNAL TIMING IS BASED ON STOP BAR DISTANCE.  
DISTANCE BETWEEN STOP BARS ON STH 13 IS 629 FEET AND DISTANCE BETWEEN STOP BAR ON STH 13 AND MOUNDVIEW DR IS 504 FT.

**TEMPORARY SIGNAL TIMING B-1-36**  
SIGNAL TIMING PROVIDED BY NC REGION TRAFFIC SECTION

STAGE 1 AND STAGE 3		STAGE 2 AND STAGE 4	
6:00 AM TO 8:00 PM	8:00 PM TO 6:00 AM	6:00 AM TO 8:00 PM	8:00 PM TO 6:00 AM
NB STH 13	NB STH 13	NB STH 13	NB STH 13
GREEN 26 SEC	GREEN 21 SEC	GREEN 30 SEC	GREEN 24 SEC
YELLOW 5 SEC	YELLOW 5 SEC	YELLOW 3 SEC	YELLOW 3 SEC
RED 22 SEC	RED 22 SEC	RED 22 SEC	RED 22 SEC
SB STH 13	SB STH 13	SB STH 13	SB STH 13
GREEN 26 SEC	GREEN 21 SEC	GREEN 30 SEC	GREEN 24 SEC
YELLOW 5 SEC	YELLOW 5 SEC	YELLOW 3 SEC	YELLOW 3 SEC
RED 22 SEC	RED 22 SEC	RED 6 SEC	RED 6 SEC

PHASES SHALL GO IN ORDER AS PRESENTED. PHASES SHALL NOT BE SKIPPED. SIGNAL TIMING IS BASED ON STOP BAR DISTANCE.  
DISTANCE BETWEEN STOP BARS ON STH 13 IS 672 FT.



Estimate Of Quantities

6140-01-65					
Line	Item	Item Description	Unit	Total	Qty
0002	203.0200	Removing Old Structure (station) 01. B-1-17 Wingwall	LS	1.000	1.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	760.000	760.000
0006	206.1000	Excavation for Structures Bridges (structure) 01. B-1-17 Wingwall	LS	1.000	1.000
0008	210.1500	Backfill Structure Type A	TON	13.000	13.000
0010	213.0100	Finishing Roadway (project) 01. 6140-01-65	EACH	1.000	1.000
0012	305.0110	Base Aggregate Dense 3/4-Inch	TON	70.000	70.000
0014	450.4000	HMA Cold Weather Paving	TON	75.000	75.000
0016	455.0605	Tack Coat	GAL	16.000	16.000
0018	460.2000	Incentive Density HMA Pavement	DOL	50.000	50.000
0020	460.5224	HMA Pavement 4 LT 58-28 S	TON	75.000	75.000
0022	502.0100	Concrete Masonry Bridges	CY	3.000	3.000
0024	502.3200	Protective Surface Treatment	SY	385.000	385.000
0026	502.3210	Pigmented Surface Sealer	SY	25.000	25.000
0028	502.4205	Adhesive Anchors No. 5 Bar	EACH	26.000	26.000
0030	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	540.000	540.000
0032	509.0200	Preparation Approaches	SY	420.000	420.000
0034	509.0301	Preparation Decks Type 1	SY	53.000	53.000
0036	509.0302	Preparation Decks Type 2	SY	23.000	23.000
0038	509.0500	Cleaning Decks	SY	385.000	385.000
0040	509.0600	Cleaning Approaches	SY	420.000	420.000
0042	509.1500	Concrete Surface Repair	SF	160.000	160.000
0044	509.2000	Full-Depth Deck Repair	SY	2.000	2.000
0046	509.2500	Concrete Masonry Overlay Decks	CY	26.000	26.000
0048	509.2600	Concrete Masonry Overlay Approaches	CY	19.000	19.000
0050	509.5100.S	Polymer Overlay	SY	385.000	385.000
0052	509.9050.S	Cleaning Parapets	LF	67.000	67.000
0054	517.1800.S	Structure Repainting Recycled Abrasive (structure) 01. B-1-36	LS	1.000	1.000
0056	517.4500.S	Negative Pressure Containment and Collection of Waste Materials (structure) 01. B-1-36	LS	1.000	1.000
0058	517.6001.S	Portable Decontamination Facility	EACH	1.000	1.000
0060	603.8000	Concrete Barrier Temporary Precast Delivered	LF	877.000	877.000
0062	603.8125	Concrete Barrier Temporary Precast Installed	LF	1,754.000	1,754.000
0064	614.0150	Anchor Assemblies for Steel Plate Beam Guard	EACH	1.000	1.000
0066	614.0905	Crash Cushions Temporary	EACH	4.000	4.000
0068	618.0100	Maintenance And Repair of Haul Roads (project) 01. 6140-01-65	EACH	1.000	1.000
0070	619.1000	Mobilization	EACH	1.000	1.000
0072	625.0100	Topsoil	SY	100.000	100.000
0074	627.0200	Mulching	SY	100.000	100.000

Estimate Of Quantities

6140-01-65

Line	Item	Item Description	Unit	Total	Qty
0076	628.1504	Silt Fence	LF	45.000	45.000
0078	628.1520	Silt Fence Maintenance	LF	45.000	45.000
0080	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0082	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0084	628.7010	Inlet Protection Type B	EACH	10.000	10.000
0086	628.7570	Rock Bags	EACH	30.000	30.000
0088	629.0210	Fertilizer Type B	CWT	6.000	6.000
0090	630.0120	Seeding Mixture No. 20	LB	2.000	2.000
0092	630.0500	Seed Water	MGAL	1.000	1.000
0094	642.5001	Field Office Type B	EACH	1.000	1.000
0096	643.0300	Traffic Control Drums	DAY	1,600.000	1,600.000
0098	643.0420	Traffic Control Barricades Type III	DAY	75.000	75.000
0100	643.0715	Traffic Control Warning Lights Type C	DAY	1,050.000	1,050.000
0102	643.0900	Traffic Control Signs	DAY	3,450.000	3,450.000
0104	643.5000	Traffic Control	EACH	1.000	1.000
0106	644.1810	Temporary Pedestrian Barricade	LF	380.000	380.000
0108	646.1020	Marking Line Epoxy 4-Inch	LF	1,500.000	1,500.000
0110	646.6464	Cold Weather Marking Epoxy 4-Inch	LF	1,232.000	1,232.000
0112	646.9000	Marking Removal Line 4-Inch	LF	1,500.000	1,500.000
0114	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	4,112.000	4,112.000
0116	649.0850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	192.000	192.000
0118	650.6500	Construction Staking Structure Layout (structure) 01. B-1-17	LS	1.000	1.000
0120	650.9910	Construction Staking Supplemental Control (project) 01. 6140-01-65	LS	1.000	1.000
0122	661.0100	Temporary Traffic Signals for Bridges (structure) 01. B-1-17	LS	1.000	1.000
0124	661.0100	Temporary Traffic Signals for Bridges (structure) 02. B-1-36	LS	1.000	1.000
0126	690.0150	Sawing Asphalt	LF	80.000	80.000
0128	690.0250	Sawing Concrete	LF	67.000	67.000
0130	715.0502	Incentive Strength Concrete Structures	DOL	500.000	500.000
0132	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	150.000	150.000
0134	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0136	SPV.0090	Special 01. Remove and Replace Guardrail	LF	50.000	50.000

3

APPROACHES								ITEMS IN CAT 0010 UNLESS OTHERWISE NOTED	
STATION - STATION	204.0120 REMOVE ASPHALTIC SURFACE MILLING SY	450.4000 HMA COLD WEATHER TON	460.5224 4 LT 58-28 S TON	455.0605 TACK COAT GAL	509.0200 PREPARATION SY	509.0600 CLEANING SY	509.2600 CONCRETE MASONRY OVERLAY CY	FINISHING ROADWAY	
18+11 - 18+51	---	---	---	---	141	141	6	213.0100 FINISHING ROADWAY	
18+97.73 - 19+42.73	---	---	---	---	171	171	7	PROJECT EACH	
86+24 - 87+24	378	37	37	8	54	54	3	6140-01-65 1	
87+66 - 88+66	382	38	38	8	54	54	3	PROJECT TOTAL 1	
PROJECT TOTALS	760	75	75	16	420	420	19		

3

BASE AGGREGATE DENSE						MAINTENANCE AND REPAIR OF HAUL ROADS		MOBILIZATIONS EROSION CONTROL			
STATION	- STATION	LOCATION	305.0110 3/4-INCH TON	REMARKS		PROJECT	618.0100 MAINTENANACE AND REPAIR OF HAUL ROADS EACH	STATION - STATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	REMARKS
86+24	- 87+24	STH 13 LT	18	B-1-17		6140-01-65	1	18+11 - 19+42	2	2	
86+24	- 87+24	STH 13 RT	19	B-1-17		PROJECT TOTAL	1	PROJECT TOTALS	2	2	
87+66	- 88+66	STH 13 LT	16	B-1-36							
87+66	- 88+66	STH 13 RT	17	B-1-36							
PROJECT TOTAL			70								

SILT FENCE					TOPSOIL, MULCHING, FERTILIZER, AND SEEDING						
STATION	LOCATION	628.1504 LF	628.1520 MAINTENANCE LF	REMARKS	STATION - STATION	LOCATION	625.0100 TOPSOIL SY	627.0200 MULCHING SY	629.0210 FERTILIZER TYPE B CWT	630.0120 SEEDING MIXTURE NO. 20 LB	630.0500 SEED WATER GAL
18+90 - 19+20	B-1-17 NW WINGWALL	45	45		18+90 - 19+20	B-1-17 NW WINGWALL	100	100	6	2	1
PROJECT TOTALS		45	45		PROJECT TOTALS		100	100	6.0	2	1

3

CRASH CUSHIONS						
(SEE TRAFFIC CONTROL SCHEDULE SHEETS FOR LOCATION)						
	614.0905 CRASH CUSHIONS TEMPORARY EACH	BACK WIDTH FT	OBJECT MARKING PATTERN	CRASH TEST LEVEL	TRAFFIC DIRECTION	TRAFFIC LOCATION
B-1-17 STAGE 1 TOTALS	1	2	OM-3R (W05-58R)	TL-3	Bidirectional	L
B-1-17 STAGE 2 TOTALS	1	2	OM-3R (W05-58R)	TL-3	Bidirectional	L
B-1-36 STAGE 1 TOTALS	1	2	OM-3R (W05-58R)	TL-3	Bidirectional	L
B-1-36 STAGE 2 TOTALS	1	2	OM-3R (W05-58R)	TL-3	Bidirectional	L
PROJECT TOTALS	4					

ITEMS IN CAT 0010 UNLESS OTHERWISE NOTED			
INLET PROTECTION			
STATION	LOCATION	628.7010 TYPE B EACH	628.7570 ROCK BAGS EACH
18+02 RT	CURB	2	6
18+27 LT	CURB	2	6
19+56 RT	CURB	2	6
19+58 LT	CURB	2	6
19+66 RT	CURB	2	6
PROJECT TOTALS		10	30

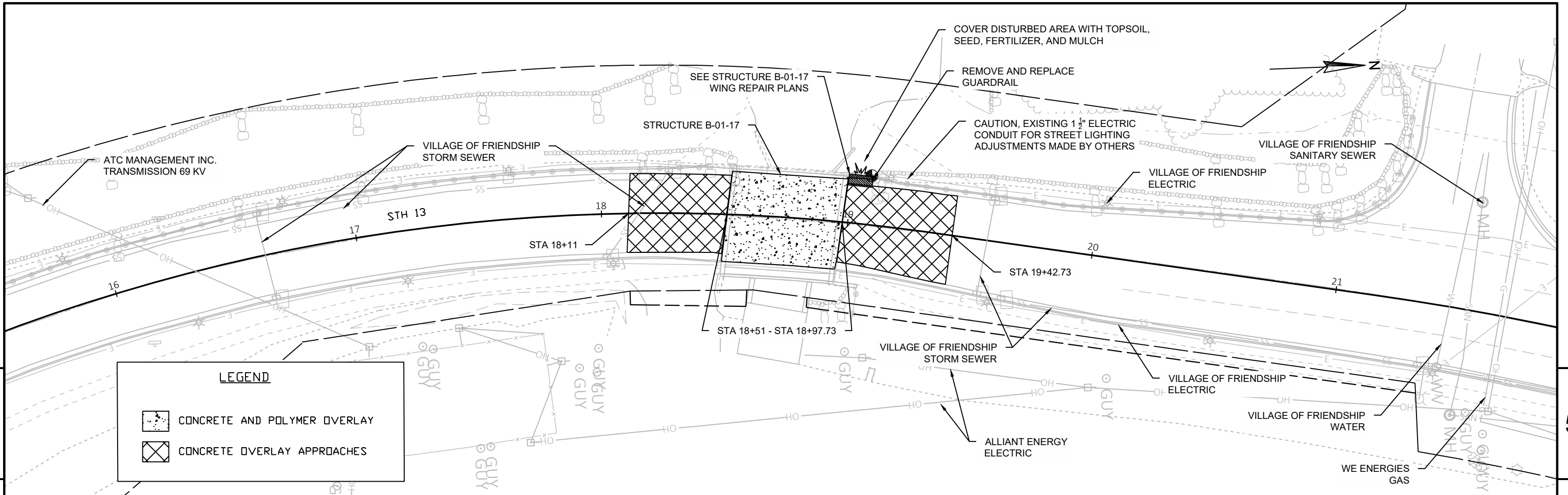
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TRAFFIC CONTROL														
(SEE TRAFFIC CONTROL SCHEDULE SHEETS FOR LOCATION AND QUANTITY BREAKDOWN)														
	STAGE DAYS	603.8000 CONCRETE BARRIER PRECAST DELIVERED LF	603.8125 CONCRETE BARRIER PRECAST INSTALLED LF	DRUMS QTY	643.0300 DRUMS DAYS	BARRICADES TYPE III QTY	643.0420 BARRICADES TYPE III DAYS	TYPE C LIGHTS QTY	643.0715 WARNING LIGHTS TYPE C DAYS	SIGNS QTY	643.0900 SIGNS DAYS	646.9000 MARKING REMOVAL 4-INCH LF	649.0150 TEMPORARY MARKING LINE 4-INCH TAPE LF	649.0850 TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18-INCH LF
ADVANCED WARNING	75	---	---	---	---	---	---	---	---	24	1800	---	---	---
B-1-17 STAGE 1 TOTALS	12	385	385	20	240	1	12	14	168	22	264	510	892	36
B-1-17 STAGE 2 TOTALS	19	--	385	16	304	1	19	14	266	22	418	722	892	12
B-1-36 STAGE 1 TOTALS	14	492	492	20	280	1	14	14	196	22	308	--	1164	24
B-1-36 STAGE 2 TOTALS	14	--	492	20	280	1	14	14	196	22	308	--	1164	24
B-1-17 STAGE 3 TOTALS	4	--	--	30	120	1	4	14	56	22	88	--	--	36
B-1-17 STAGE 4 TOTALS	4	--	--	26	104	1	4	14	56	22	88	--	--	12
B-1-36 STAGE 3 TOTALS	4	--	--	34	136	1	4	14	56	22	88	100	--	24
B-1-36 STAGE 4 TOTALS	4	--	--	34	136	1	4	14	56	22	88	168	--	24
PROJECT TOTALS		877	1754		1600		75		1050		3450	1500	4112	192

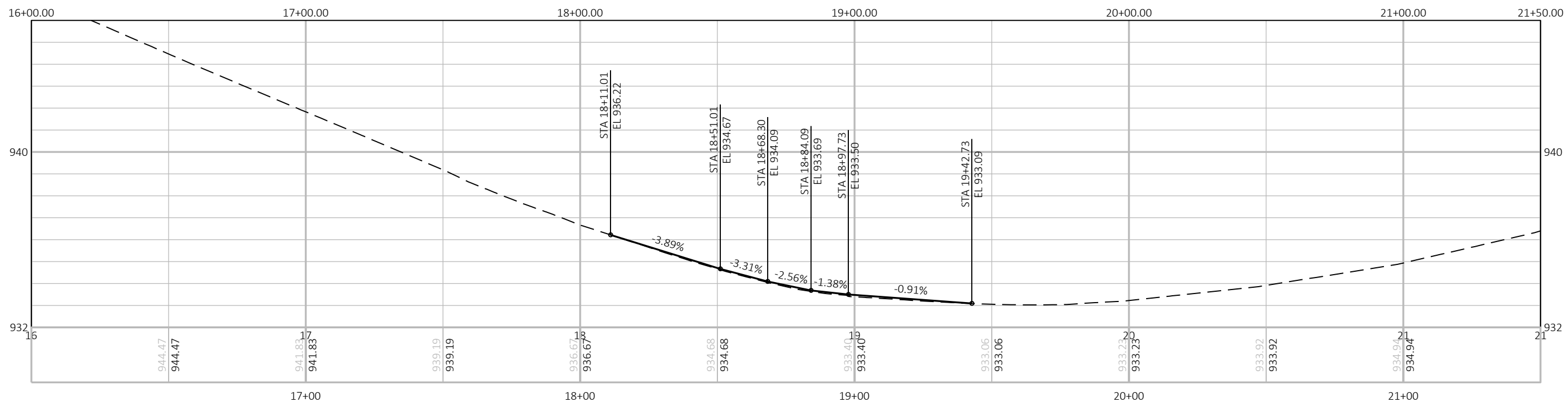
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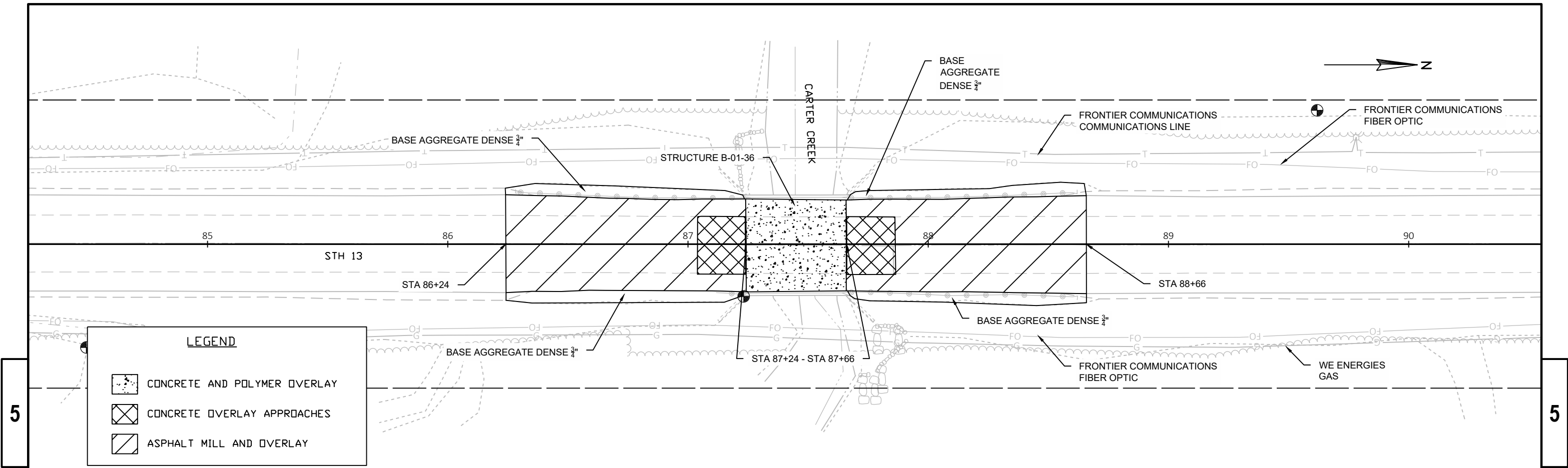
<div>FIELD OFFICE</div> <table><tr><td></td><td>642.5001</td></tr><tr><td>PROJECT</td><td>TYPE B</td></tr><tr><td>6140-01-65</td><td>EACH</td></tr><tr><td>1</td><td></td></tr><tr><td colspan="2">PROJECT TOTAL</td></tr><tr><td>1</td><td></td></tr></table>			642.5001	PROJECT	TYPE B	6140-01-65	EACH	1		PROJECT TOTAL		1		<div>PEDESTRIAN BARRICADE</div> <table><tr><td></td><td></td><td>644.1810</td><td></td></tr><tr><td>STATION - STATION</td><td>LOCATION</td><td>LF</td><td>REMARKS</td></tr><tr><td>17+13 - 20+93</td><td>SIDEWALK</td><td>380</td><td></td></tr><tr><td colspan="2">PROJECT TOTAL</td><td>380</td><td></td></tr></table>				644.1810		STATION - STATION	LOCATION	LF	REMARKS	17+13 - 20+93	SIDEWALK	380		PROJECT TOTAL		380		<div>ITEMS IN CAT 0010 UNLESS OTHERWISE NOTED</div> <div>MARKING</div> <table><tr><td></td><td></td><td>646.1020</td><td>646.1020</td><td>646.6464</td></tr><tr><td></td><td></td><td>LINE EPOXY</td><td>LINE EPOXY</td><td>COLD WEATHER</td></tr><tr><td></td><td></td><td>4-INCH</td><td>4-INCH</td><td>MARKING EPOXY</td></tr><tr><td></td><td></td><td>WHITE</td><td>YELLOW</td><td>4-INCH</td></tr><tr><td>STATION - STATION</td><td>LOCATION</td><td>LF</td><td>LF</td><td>LF</td></tr><tr><td>18+11 - 19+43</td><td>STAGE 2</td><td>---</td><td>264</td><td>264</td></tr><tr><td>86+24 - 88+66</td><td>STAGE 2</td><td>484</td><td>484</td><td>968</td></tr><tr><td>18+11 - 19+43</td><td>STAGE 4</td><td>---</td><td>100</td><td></td></tr><tr><td>86+24 - 88+66</td><td>STAGE 4</td><td>84</td><td>84</td><td></td></tr><tr><td colspan="2">SUB TOTALS</td><td>568</td><td>932</td><td></td></tr><tr><td colspan="2">PROJECT TOTAL</td><td></td><td>1500</td><td>1232</td></tr></table>					646.1020	646.1020	646.6464			LINE EPOXY	LINE EPOXY	COLD WEATHER			4-INCH	4-INCH	MARKING EPOXY			WHITE	YELLOW	4-INCH	STATION - STATION	LOCATION	LF	LF	LF	18+11 - 19+43	STAGE 2	---	264	264	86+24 - 88+66	STAGE 2	484	484	968	18+11 - 19+43	STAGE 4	---	100		86+24 - 88+66	STAGE 4	84	84		SUB TOTALS		568	932		PROJECT TOTAL			1500	1232
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<div>TRAFFIC CONTROL</div> <table><tr><td></td><td>643.5000</td></tr><tr><td>PROJECT</td><td>EACH</td></tr><tr><td>6140-01-65</td><td>1</td></tr><tr><td colspan="2">PROJECT TOTAL</td></tr><tr><td colspan="2">1</td></tr></table>			643.5000	PROJECT	EACH	6140-01-65	1	PROJECT TOTAL		1		<div>CONSTRUCTION STAKING</div> <table><tr><td></td><td>650.6500</td><td>650.9910</td></tr><tr><td></td><td>STRUCTURE</td><td>SUPPELMENTAL</td></tr><tr><td></td><td>LAYOUT</td><td>CONTROL</td></tr><tr><td>LOCATION</td><td>LS</td><td>LS</td></tr><tr><td>PROJECT</td><td>---</td><td>1</td></tr><tr><td>B-1-17</td><td>1</td><td>---</td></tr><tr><td colspan="2">PROJECT TOTAL</td><td>1</td></tr></table>			650.6500	650.9910		STRUCTURE	SUPPELMENTAL		LAYOUT	CONTROL	LOCATION	LS	LS	PROJECT	---	1	B-1-17	1	---	PROJECT TOTAL		1	<div>TEMPORARY TRAFFIC SIGNALS</div> <table><tr><td></td><td></td><td>661.0100.01</td><td>661.0100.02</td></tr><tr><td></td><td></td><td>FOR</td><td>FOR</td></tr><tr><td></td><td></td><td>BRIDGES</td><td>BRIDGES</td></tr><tr><td></td><td></td><td>(STRUCTURE B-1-17)</td><td>(STRUCTURE B-1-36)</td></tr><tr><td>STATION - STATION</td><td>LOCATION</td><td>LS</td><td>LS</td></tr><tr><td>15+86 - 20+97</td><td>B-1-17</td><td>1</td><td>---</td></tr><tr><td>83+84 - 91+06</td><td>B-1-36</td><td>---</td><td>1</td></tr><tr><td colspan="2">PROJECT TOTAL</td><td>1</td><td>1</td></tr></table>					661.0100.01	661.0100.02			FOR	FOR			BRIDGES	BRIDGES			(STRUCTURE B-1-17)	(STRUCTURE B-1-36)	STATION - STATION	LOCATION	LS	LS	15+86 - 20+97	B-1-17	1	---	83+84 - 91+06	B-1-36	---	1	PROJECT TOTAL		1	1																				
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<div>SAWING ASPHALT</div> <table><tr><td></td><td></td><td>690.0150</td><td></td></tr><tr><td>STATION</td><td>LOCATION</td><td>LF</td><td>REMARKS</td></tr><tr><td>86+24</td><td></td><td>40</td><td></td></tr><tr><td>88+66</td><td></td><td>40</td><td></td></tr><tr><td colspan="2">PROJECT TOTAL</td><td>80</td><td></td></tr></table>				690.0150		STATION	LOCATION	LF	REMARKS	86+24		40		88+66		40		PROJECT TOTAL		80		<div>SAWING CONCRETE</div> <table><tr><td></td><td></td><td>690.0250</td><td></td></tr><tr><td>STATION - STATION</td><td>LOCATION</td><td>LF</td><td>REMARKS</td></tr><tr><td>18+11</td><td></td><td>32</td><td></td></tr><tr><td>19+42.73</td><td></td><td>35</td><td></td></tr><tr><td colspan="2">PROJECT TOTAL</td><td>67</td><td></td></tr></table>				690.0250		STATION - STATION	LOCATION	LF	REMARKS	18+11		32		19+42.73		35		PROJECT TOTAL		67		<div>REMOVE AND REPLACE GUARDRAIL</div> <table><tr><td></td><td>SPV.0090.01</td></tr><tr><td></td><td>REMOVE AND</td></tr><tr><td></td><td>REPLACE</td></tr><tr><td></td><td>GUARDRAIL</td></tr><tr><td>LOCATION</td><td>LF</td></tr><tr><td>B-1-17 NW WINGWALL</td><td>50</td></tr><tr><td colspan="2">PROJECT TOTAL</td></tr><tr><td colspan="2">50</td></tr></table>				SPV.0090.01		REMOVE AND		REPLACE		GUARDRAIL	LOCATION	LF	B-1-17 NW WINGWALL	50	PROJECT TOTAL		50																												
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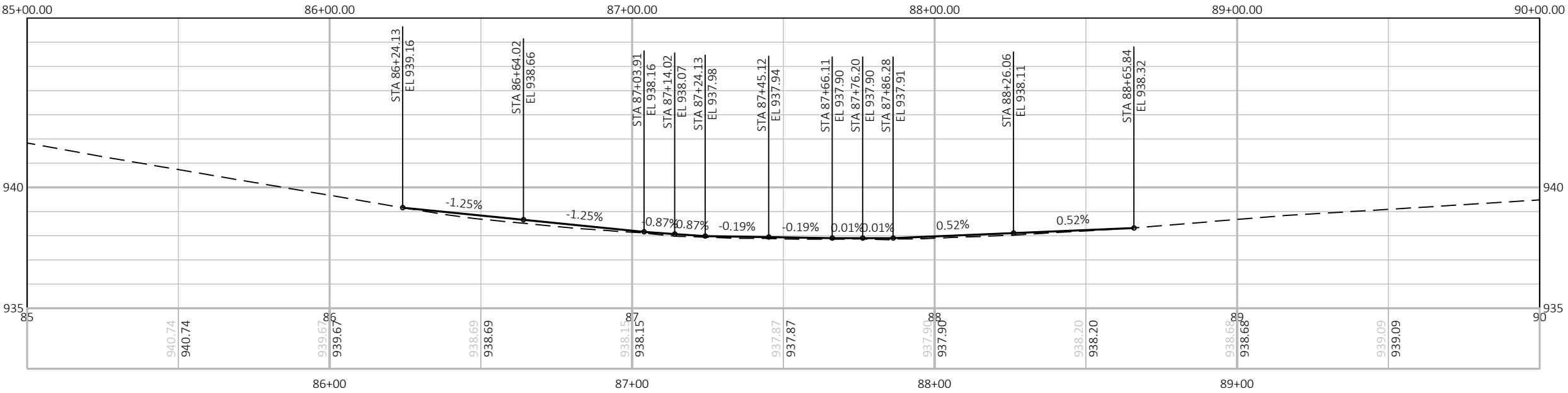


Profile View of 13 South  
V Scale : 5  
H Scale : 40  
Exaggeration : 8



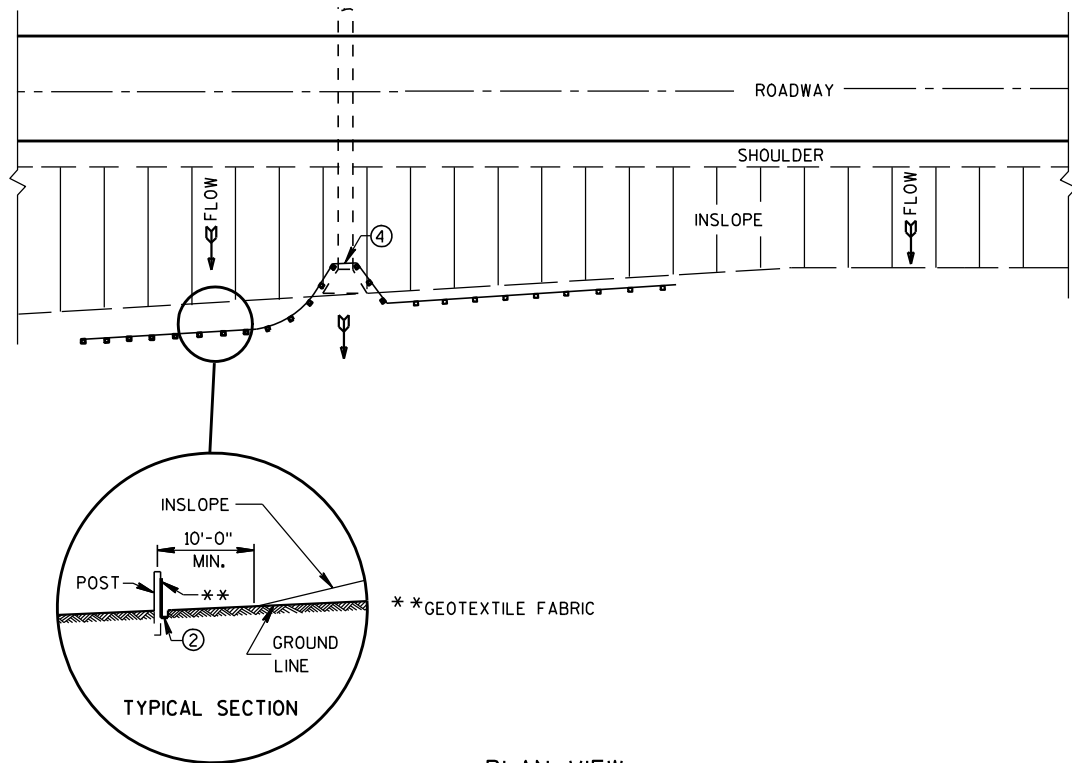


Profile View of 13 North  
V Scale : 5  
H Scale : 40  
Exaggeration : 8

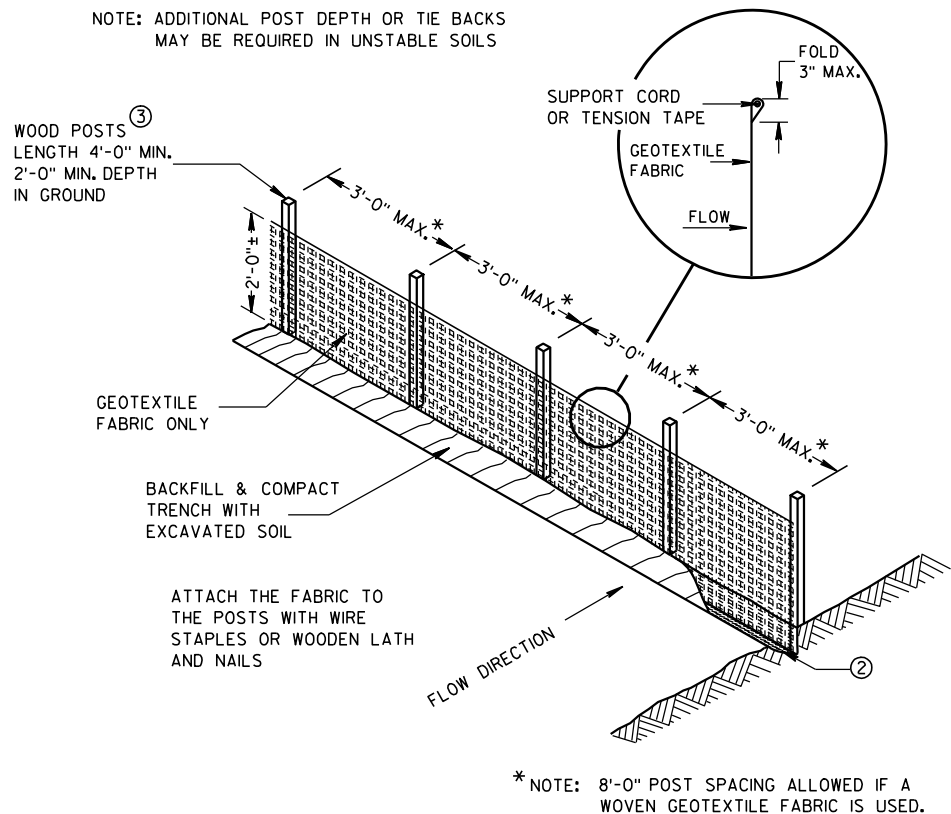


Standard Detail Drawing List

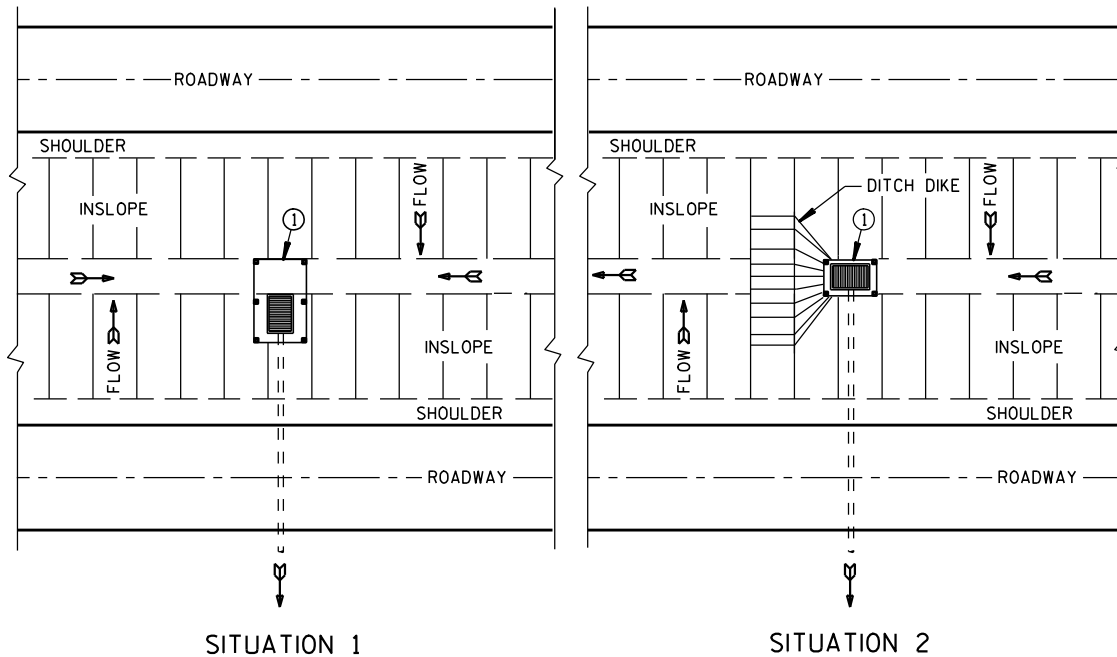
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09G02-05A	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
09G02-05B	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
09G02-05C	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
14B07-15A	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15B	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15C	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15D	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15E	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15F	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15G	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15H	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-15I	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B08-02A	CRASH CUSHION/SAND BARREL ARRAY AND OTHER TEMPORARY BARRIER LAYOUT DETAILS
14B08-02B	CRASH CUSHION/SAND BARREL ARRAY AND OTHER TEMPORARY BARRIER LAYOUT DETAILS
14B08-02C	CRASH CUSHION/SAND BARREL ARRAY AND OTHER TEMPORARY BARRIER LAYOUT DETAILS
14B08-02D	CRASH CUSHION/SAND BARREL ARRAY AND OTHER TEMPORARY BARRIER LAYOUT DETAILS
14B08-02E	CRASH CUSHION/SAND BARREL ARRAY AND OTHER TEMPORARY BARRIER LAYOUT DETAILS
15C02-08F	ADVANCED WIDTH RESTRICTION SIGNING
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C11-08B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15D30-06A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-06B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-06C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D33-06	TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



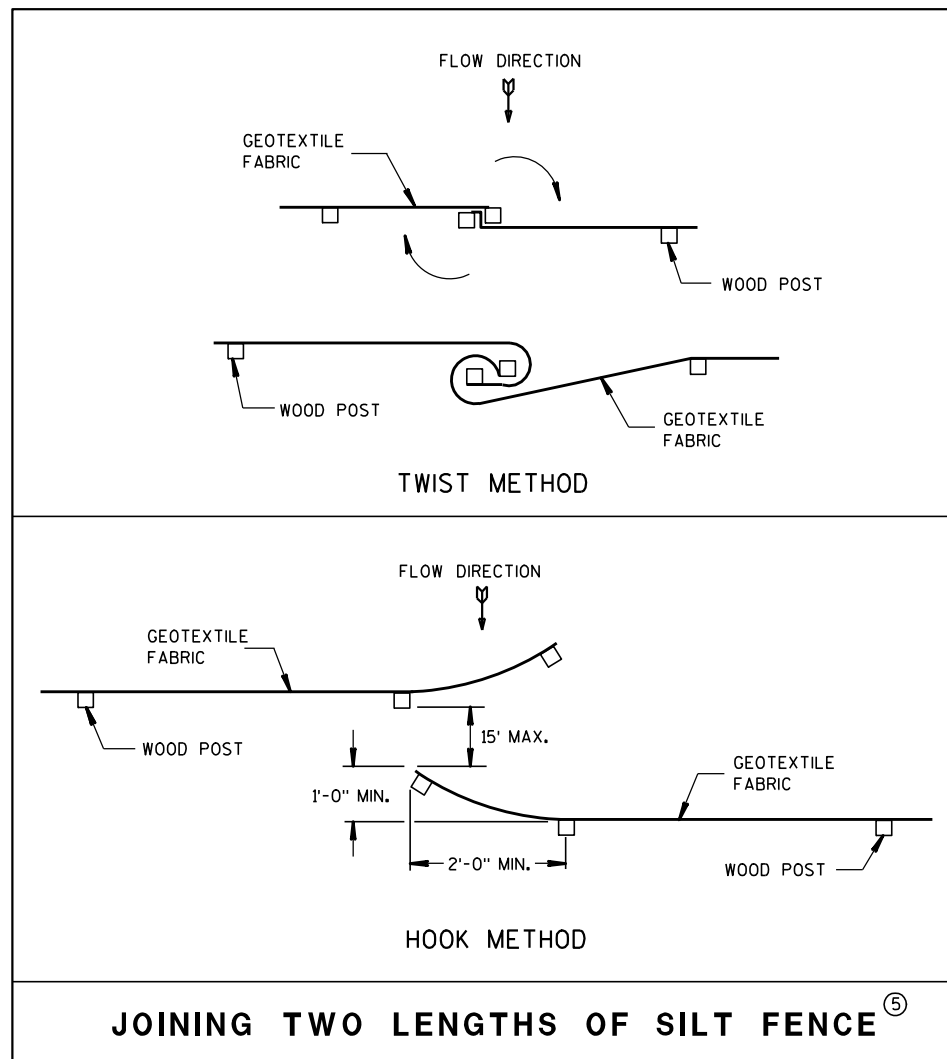
PLAN VIEW  
TYPICAL APPLICATION OF SILT FENCE



SILT FENCE



PLAN VIEW  
SILT FENCE AT MEDIAN SURFACE DRAINS

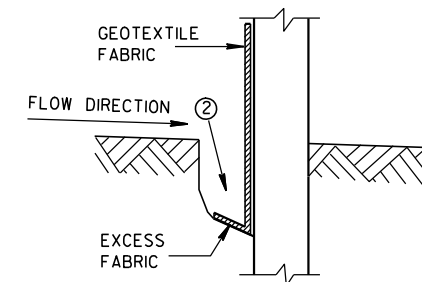


JOINING TWO LENGTHS OF SILT FENCE<sup>⑤</sup>

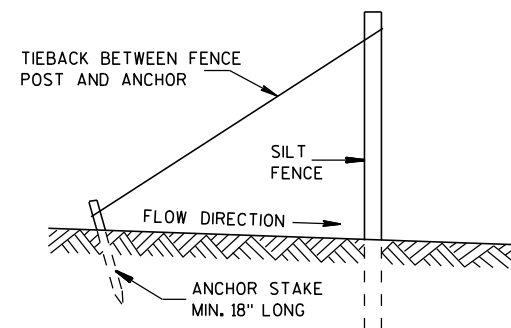
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

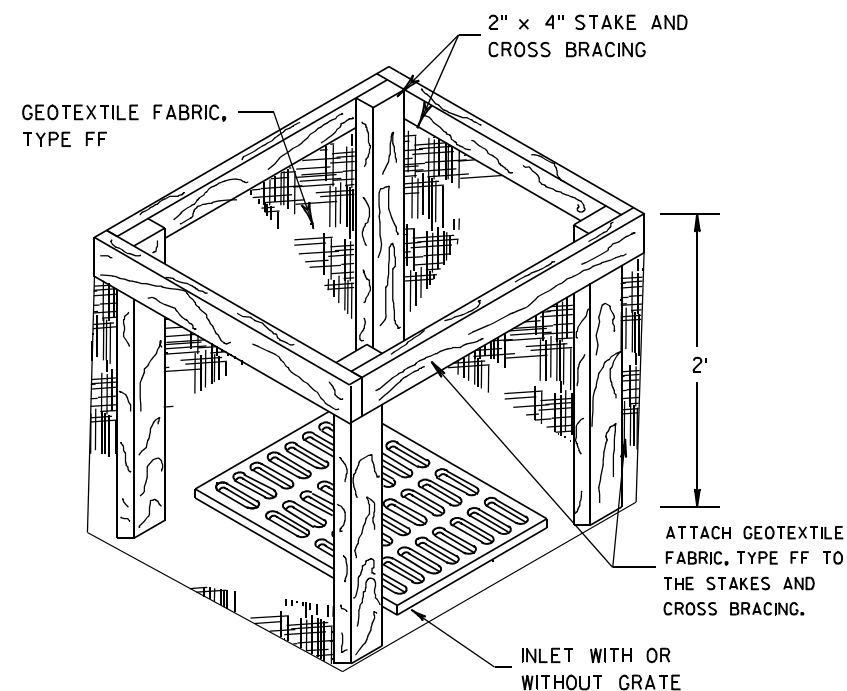
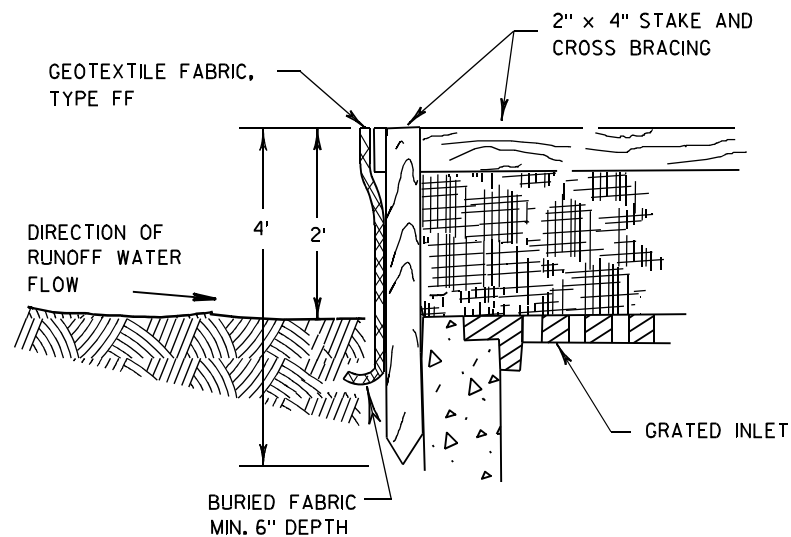


TRENCH DETAIL



SILT FENCE TIE BACK  
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



**INLET PROTECTION, TYPE A**

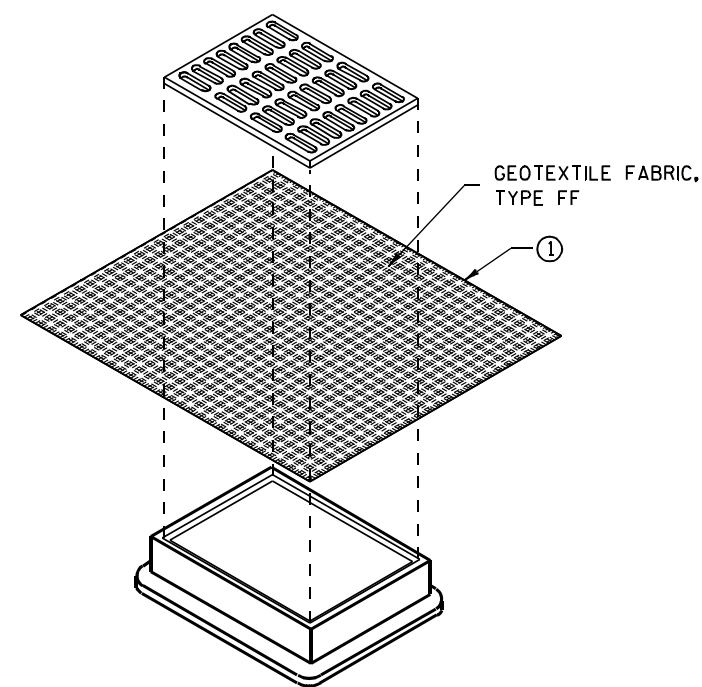
**GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

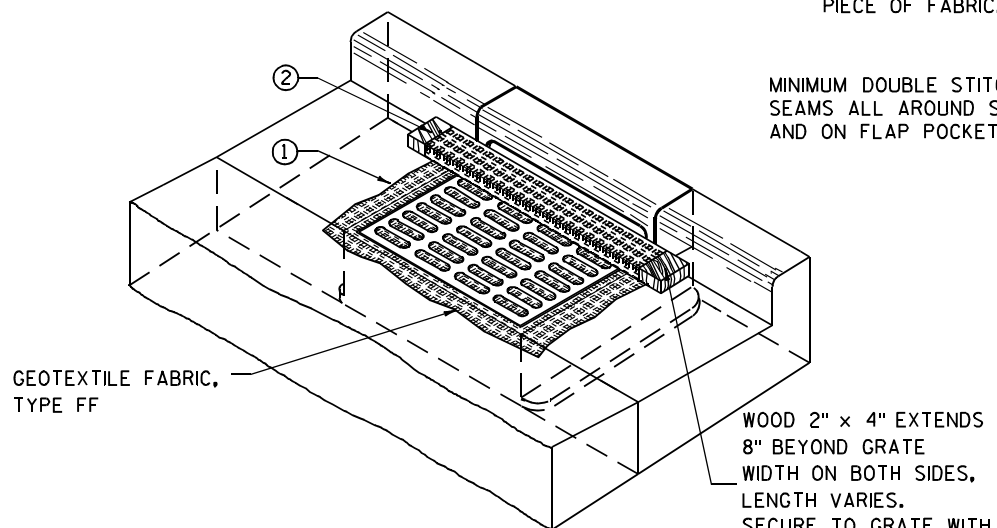
MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B  
(WITHOUT CURB BOX)**  
(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



**INLET PROTECTION, TYPE C (WITH CURB BOX)**

**INSTALLATION NOTES**

**TYPE B & C**

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

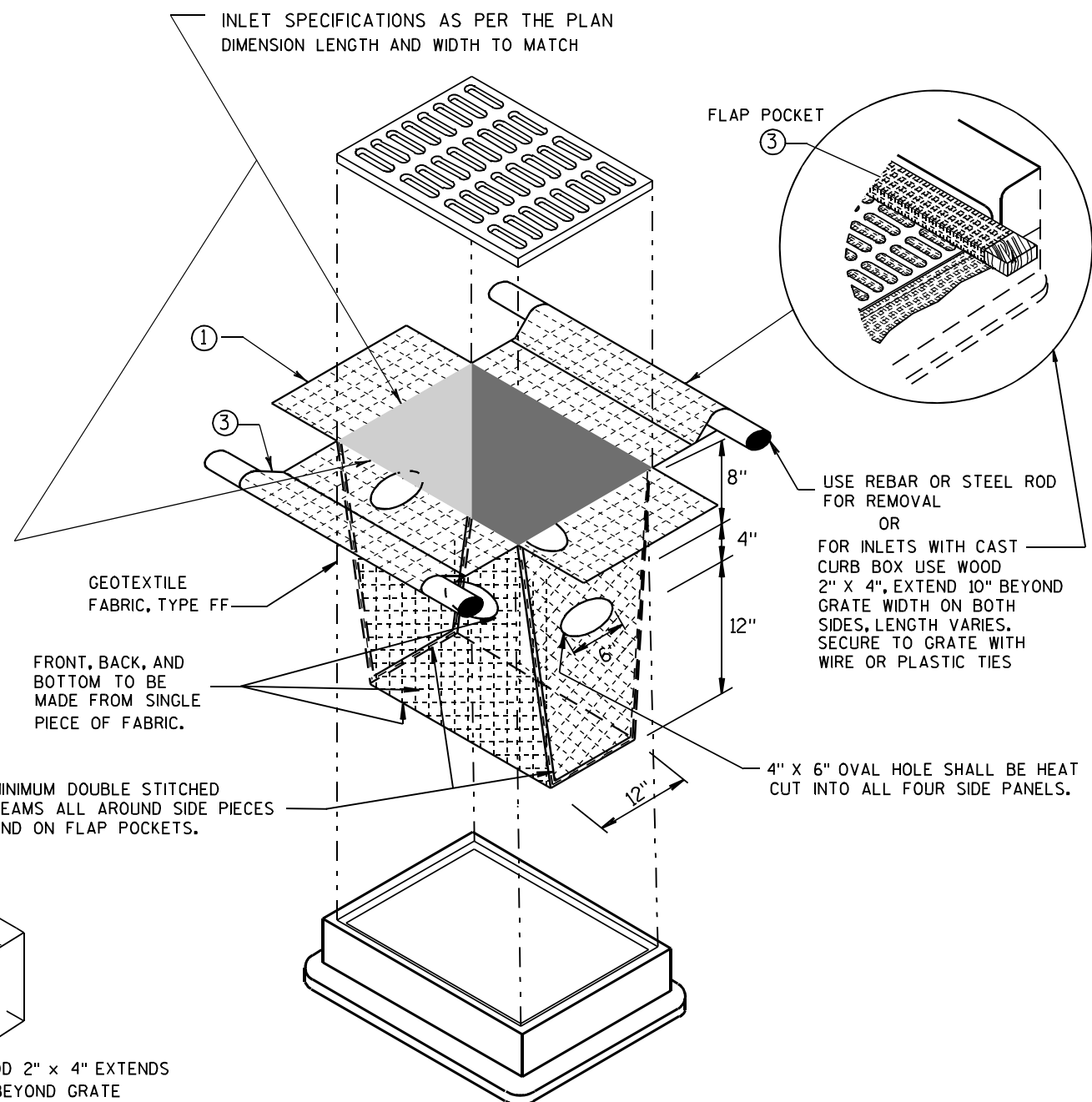
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

**TYPE D**

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



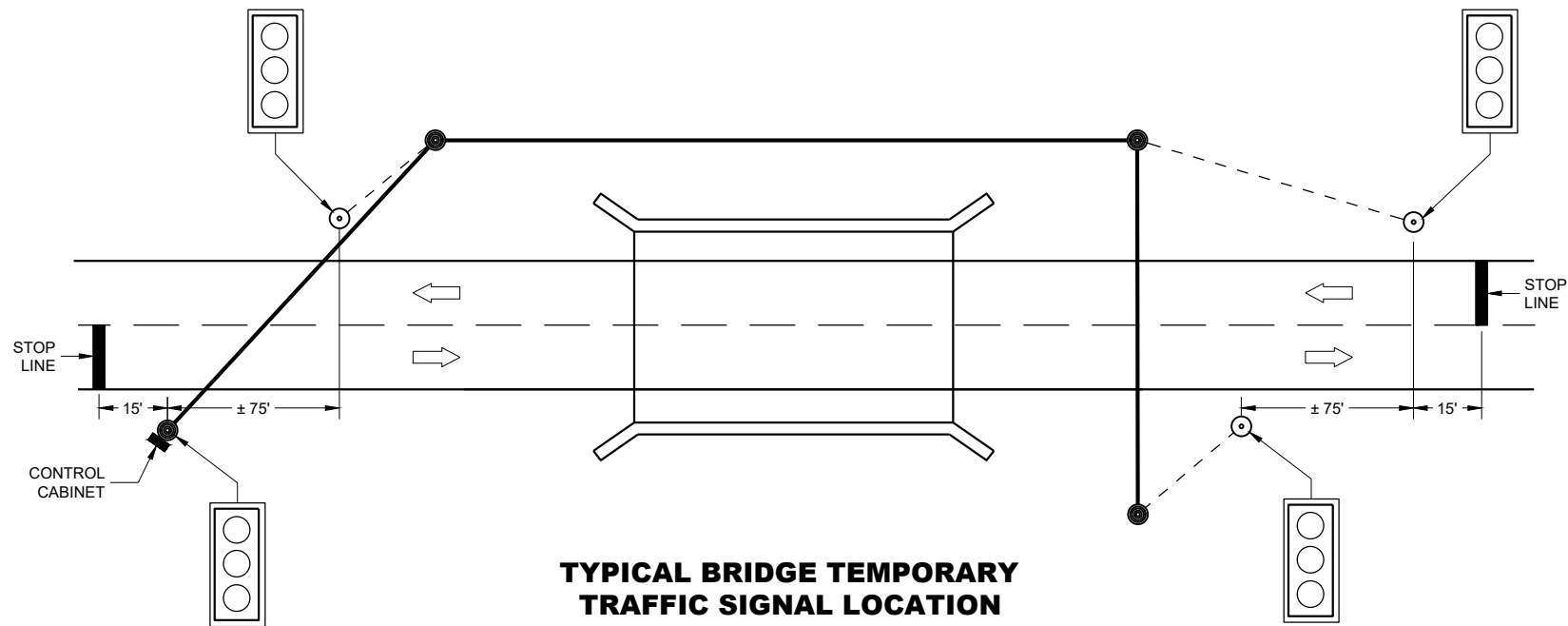
**INLET PROTECTION, TYPE D**

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION  
TYPE A, B, C, AND D**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
10/16/02 /S/ Beth Cannestra  
DATE  
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER



TYPICAL BRIDGE TEMPORARY TRAFFIC SIGNAL LOCATION

LEGEND

- WOOD POLE (NON-BREAKAWAY)
- WOOD POST (BREAKAWAY)
- SIGNAL CABLE
- SIGNAL CABLE W/MESSENGER
- DIRECTION OF TRAFFIC
- LED TRAFFIC SIGNAL WITH BACKPLATE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

POLE MOUNTED TRAFFIC SIGNAL CONTROL CABINET MAY BE MOUNTED ON THE SERVICE POLE IF THE ELECTRICAL UTILITY ALLOWS THE INSTALLATION.

WHEN UTILITY POLES ARE USED TO SPAN THE TEMPORARY OVERHEAD CABLE, WRITTEN PERMISSION MUST BE OBTAINED FROM THE OWNER OF THE POLES AND GIVEN TO THE PROJECT MANAGER. ALL PERTINENT UTILITY AND CODE CLEARANCES SHALL BE MAINTAINED.

WOOD POLES (NON-BREAKAWAY) SHALL BE NO CLOSER TO EDGE OF PAVEMENT THAN OFFSET DISTANCE CHART ALLOWS OR 4 FEET BEHIND PROTECTIVE BARRIER (BEAM GUARD, ETC.).

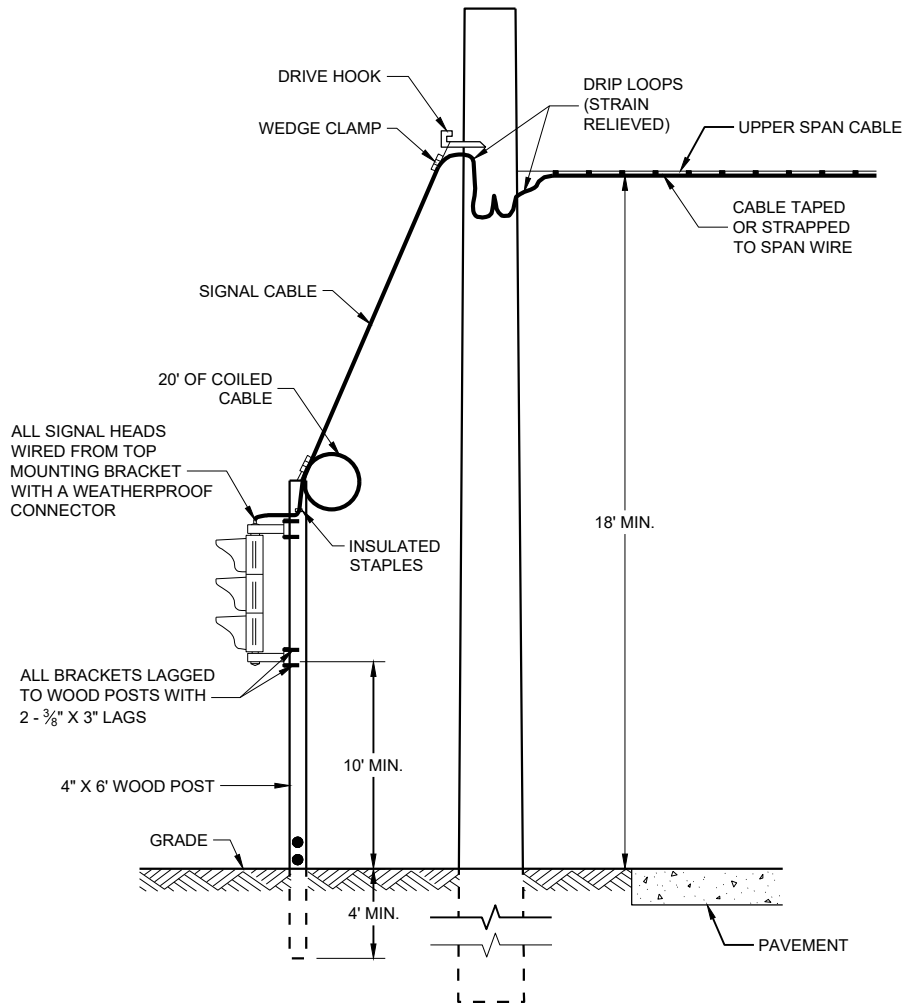
WOOD POSTS (BREAKAWAY) SHALL BE NO CLOSER THAN 2 FEET OUTSIDE OF SHOULDER.

VERTICAL CLEARANCE ETC. PER NEC.

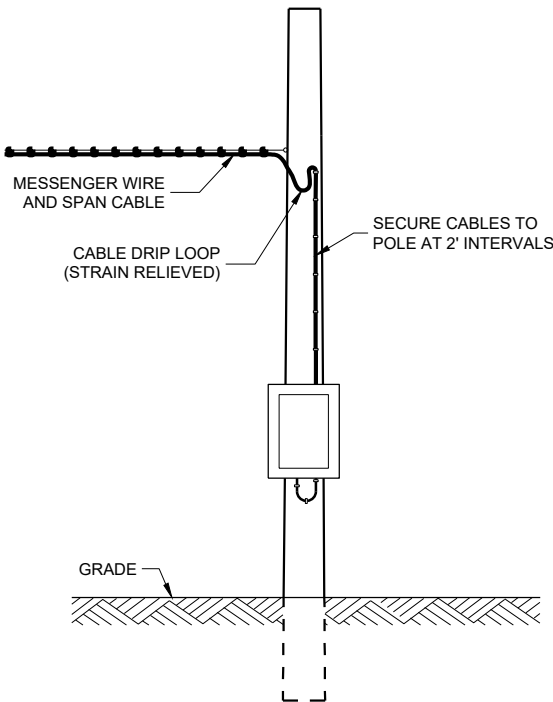
TRAFFIC SIGNAL FACES SHALL BE TYPICALLY PLACED 12 FEET FROM EDGE OF PAVEMENT.

EACH TRAFFIC SIGNAL SHALL HAVE A BACKPLATE.

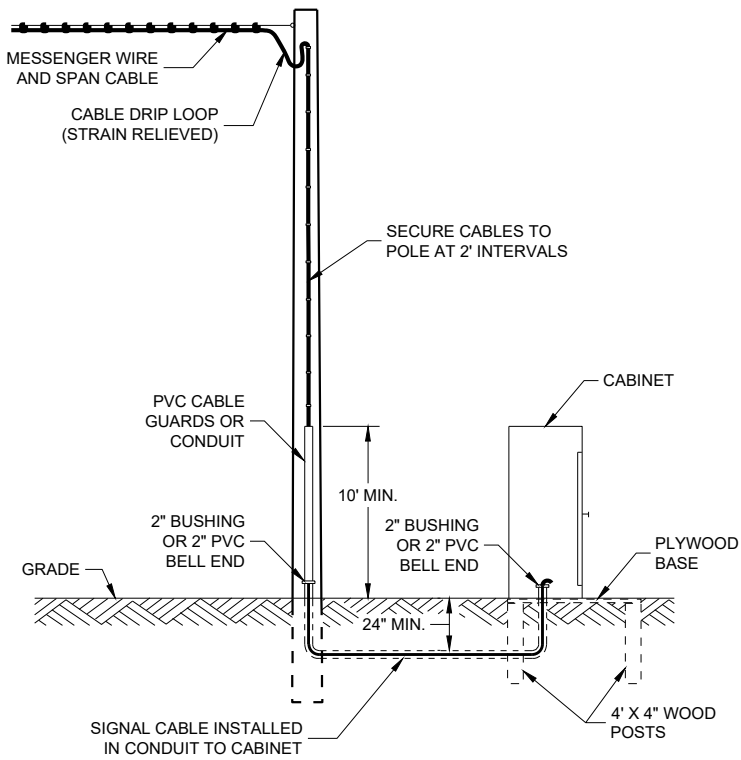
SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15D33.



TYPICAL DROP TO TRAFFIC SIGNAL FACE



POLE MOUNT CABINET INSTALLATION



GROUND MOUNT CABINET INSTALLATION

MINIMUM POLE LENGTHS	CLASS	POLE BURIAL DEPTHS
25'	V	5'
30'	V	6'
35'	IV	7'
40'	IV	8'
45'	IV	9'

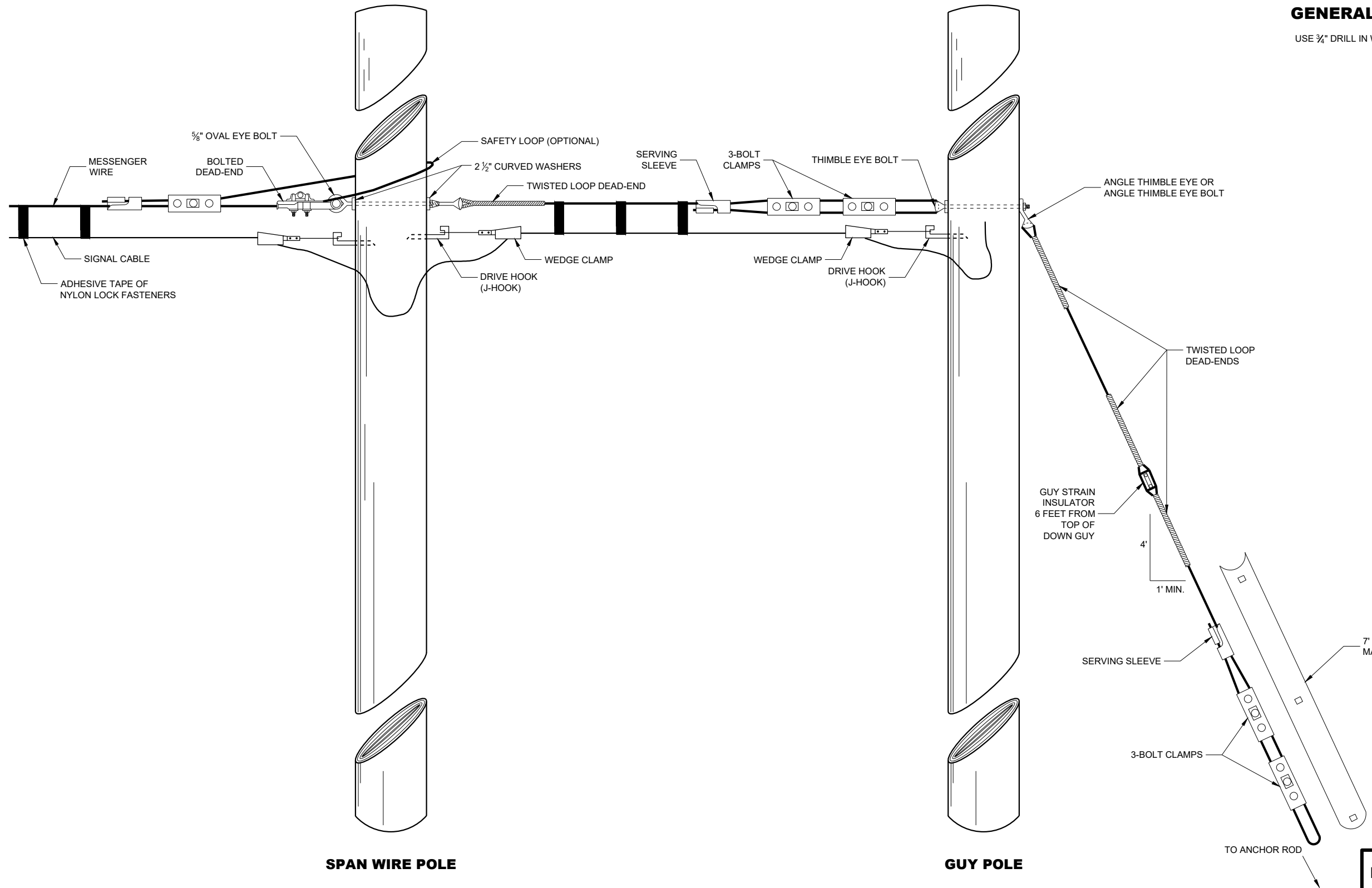
OFFSET DISTANCES FOR TEMPORARY NON-BREAKAWAY POLES	
SPEED LIMIT	OFFSET DISTANCE*
GREATER THAN 45 MPH	18 FT
45 MPH OR LESS	12 FT
45 MPH OR LESS W/CURBS	2 FT

\* NOTE: OFFSET MEASURED FROM OUTER EDGE OF OUTSIDE THRU LANE.

BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

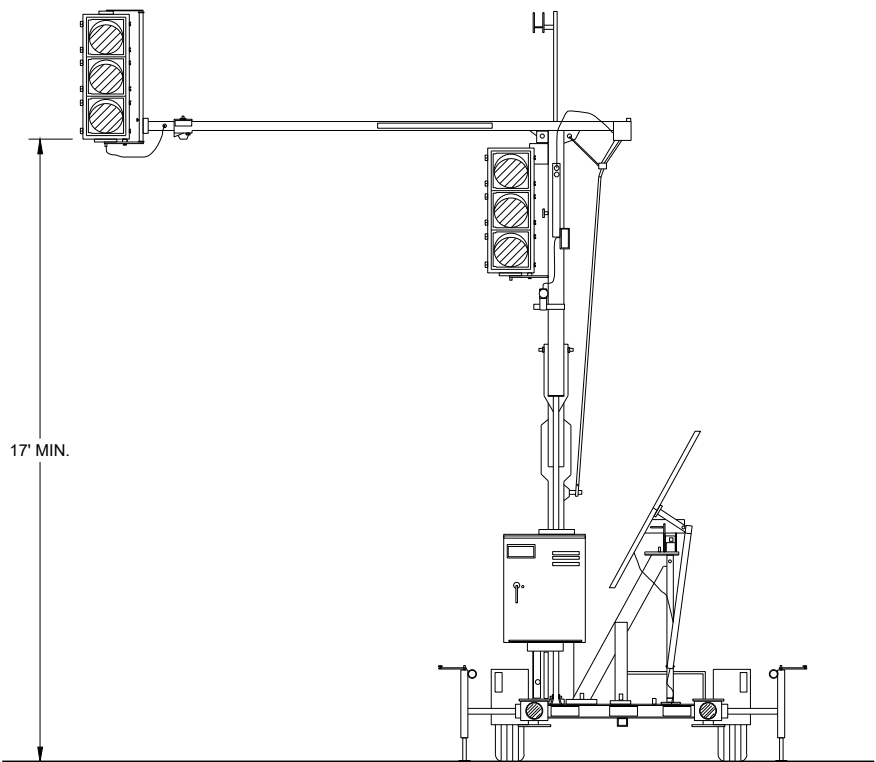
APPROVED  
March 2018 /S/ Ahmet Demirelek  
DATE ROADWAY STANDARDS DEVELOPMENT  
FHWA ENGINEER



**GENERAL NOTES**  
USE 3/4\"/>

**TYPICAL DEAD-ENDINGS OR GUYING**

<b>BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2015 DATE	/S/ Ahmet Demerbilek ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	

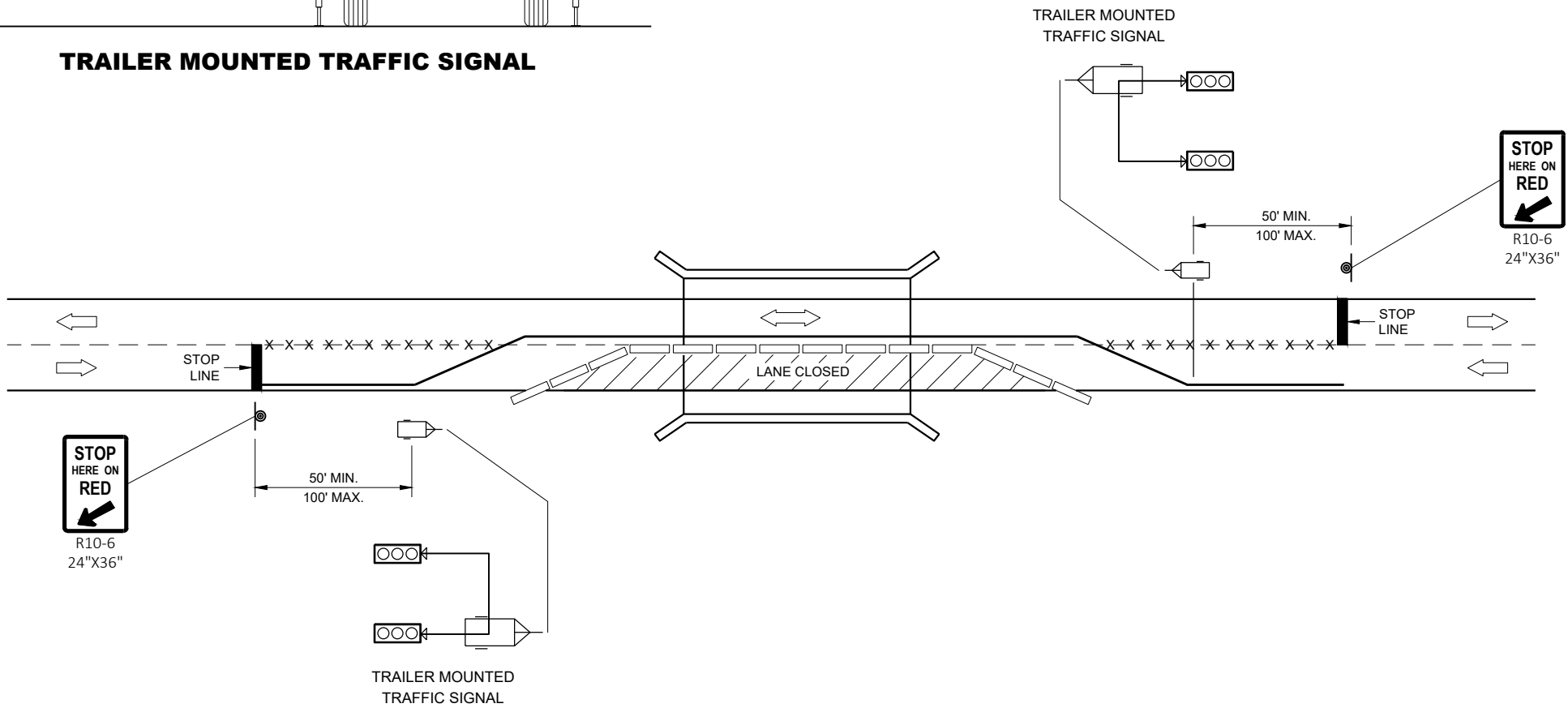


TRAILER MOUNTED TRAFFIC SIGNAL

GENERAL NOTES

DETAIL OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15D33.



TYPICAL TRAILER MOUNTED TRAFFIC SIGNAL LOCATION

LEGEND

- POST MOUNTED SIGN
- TEMPORARY PRECAST CONCRETE BARRIER
- TRAILER MOUNTED TRAFFIC SIGNAL
- REMOVE PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC

BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

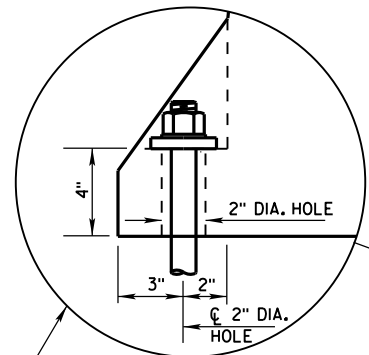
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
June 2015  
DATE

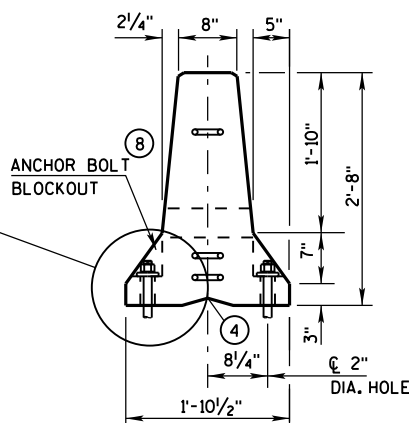
/S/ Ahmet Demerbilek  
ROADWAY STANDARDS DEVELOPMENT  
ENGINEER

FHWA

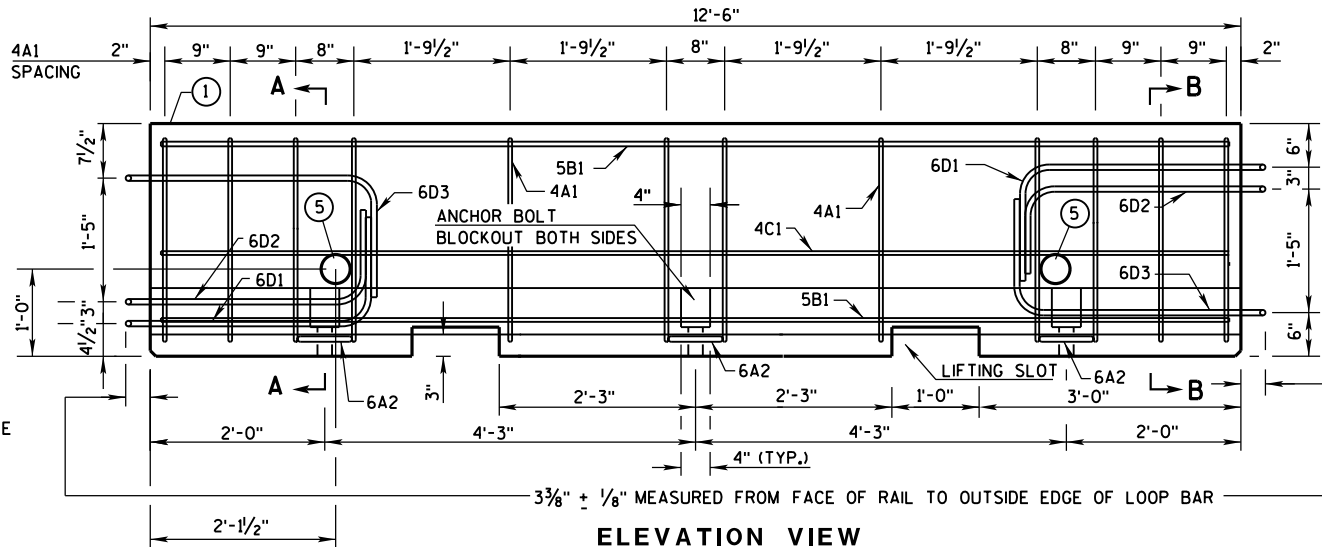




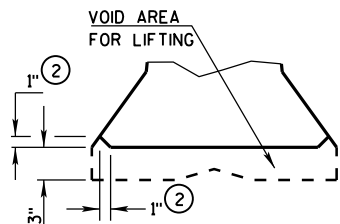
ANCHOR ON TRAFFIC SIDE  
ONLY WHEN REQUIRED  
(SEE SHEET D FOR ADDITIONAL  
ANCHOR DETAIL)



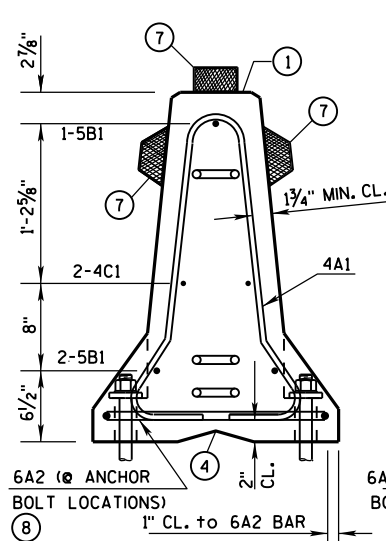
END VIEW



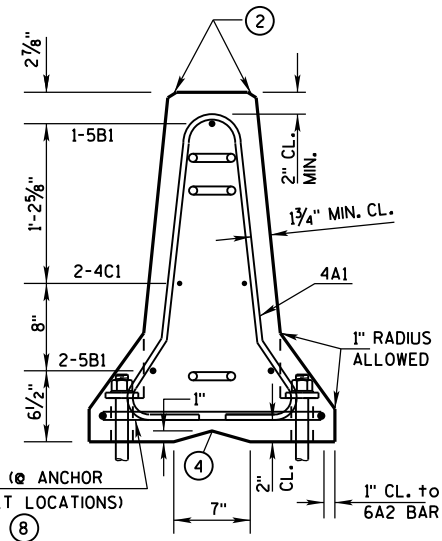
ELEVATION VIEW



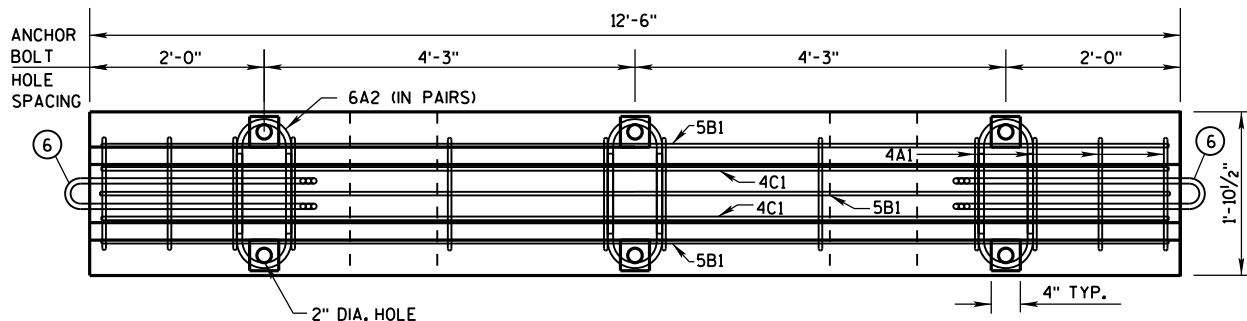
DETAIL "B"  
LIFTING SLOT DETAIL



SECTION A-A  
(STIRRUP PLACEMENT)

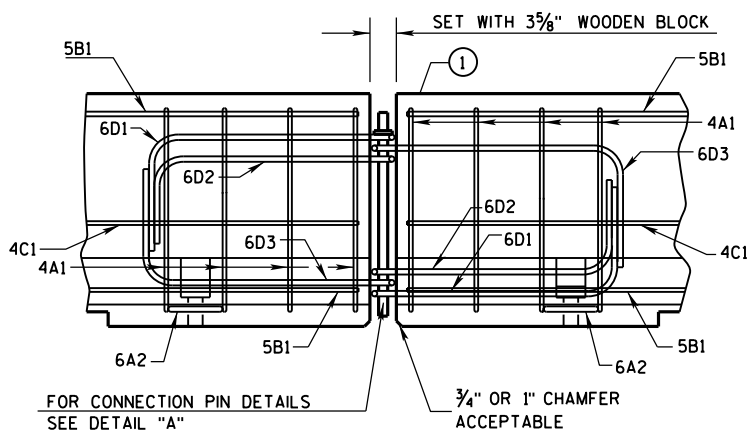


SECTION B-B  
(STIRRUP PLACEMENT)

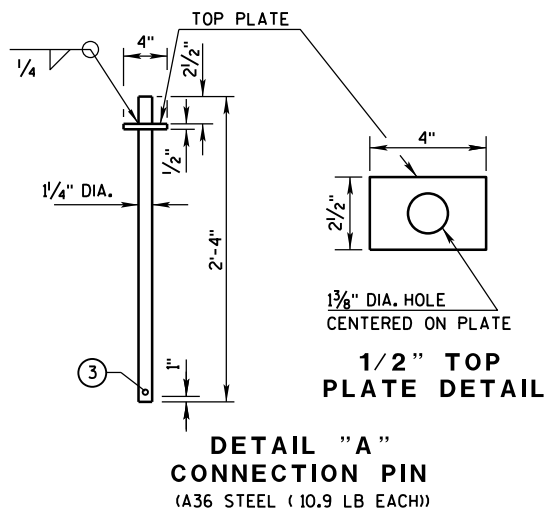


PLAN VIEW

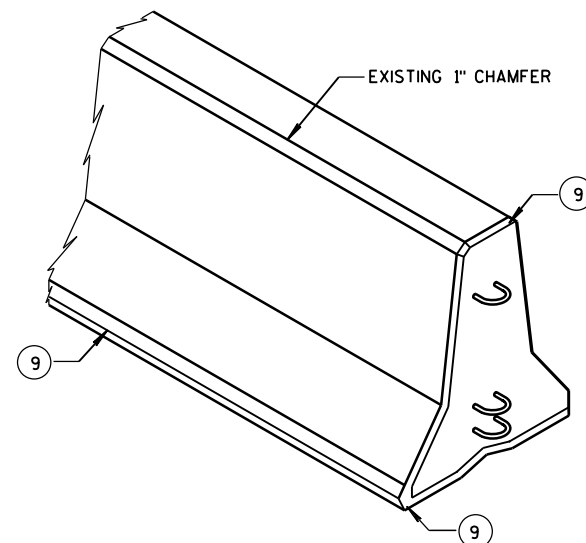
## DETAILS OF BARRIER SECTION



DETAILS OF BARRIER CONNECTION



DETAIL "A"  
CONNECTION PIN  
(A36 STEEL (10.9 LB EACH))



## GENERAL NOTES

THESE GENERAL NOTES APPLY TO SHEETS 14B7-15(a) THRU 14B7-15(i).

DO NOT INTERMIX CONCRETE BARRIER TEMPORARY PRECAST, 12'-6" (CBTP12.5) WITH OTHER TEMPORARY CONCRETE BARRIERS.

USE ASTM A-615, GRADE 60, DEFORMED STEEL BARS FOR BARS 4A1, 6A2, 5B1 AND 4C1 IN THE BARRIER SECTION AND FOR 4V1, 4V2, 4V3, 4V4, 4V5, 4V6, 4F1, 4F2 AND 5F3 IN THE BARRIER TAPER SECTION.

LOOP BARS 6D1, 6D2 AND 6D3 SHALL BE 3/4" SMOOTH STEEL BARS WITH A MINIMUM YIELD STRENGTH OF 60 KSI, A TENSILE STRENGTH OF NOT LESS THAN 1.25 TIMES THE YIELD STRENGTH BUT A MINIMUM OF 80 KSI, A MINIMUM 14% ELONGATION IN 8 INCHES AND PASSING A 180 DEGREE BEND TEST USING A 3-1/2" PIN BEND DIAMETER FOR BEND TESTS. THE LOOPS SHALL BE INSTALLED WITHIN 1/8" OF THE PLAN DIMENSION.

CONSTRUCT LIFTING SLOTS AS SPECIFIED ON THE PLANS TO FACILITATE THE DRAINAGE OF WATER AFTER INSTALLATION.

PLACE BARRIER ON A PAVED SURFACE. REMOVE ALL LOOSE DIRT AND SAND FROM THE ROADWAY SURFACE PRIOR TO PLACEMENT OF THE BARRIER.

INSTALL MECHANICAL OR ADHESIVE ANCHORS PER MANUFACTURER'S RECOMMENDATIONS. PROVIDE MANUFACTURER'S INFORMATION TO PROJECT ENGINEER.

- MARK ONE END OF EACH BARRIER PERMANENTLY BY FORMING INTO THE BARRIER THE FOLLOWING INFORMATION:
  - TYPE: WICBTP
  - MANUFACTURER
  - DATE MANUFACTURED (MONTH AND YEAR)
- 1" CHAMFER TO PREVENT SPALLING.
- A 3/8" HOLE IN THE CONNECTION PIN, AT THE LOCATION SHOWN, IS ACCEPTABLE, BUT NOT REQUIRED..
- "V" NOTCH IS OPTIONAL.
- THE 4" DIAMETER, 11 GAUGE STEEL, ROUND MECHANICAL TUBING SLEEVE FOR LIFTING (OPTIONAL).
- NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.
- USE DELINEATORS CONFORMING TO SECTION 633 OF THE STANDARD SPECIFICATIONS. CONTRACTOR MAY USE ALTERNATE SHAPES AND HOUSING. INSTALL DELINEATORS ACCORDING TO MANUFACTURES INSTRUCTION. INSTALL YELLOW REFLECTORS WHEN BARRIER IS LOCATED TO THE LEFT OF TRAFFIC AND WHITE REFLECTORS WHEN BARRIER IS LOCATED TO THE RIGHT OF TRAFFIC. SPACE DELINEATORS A MAXIMUM OF 25 FEET APART. PROVIDE TOP MOUNTED DELINEATORS IN ADDITION TO THE SIDE MOUNTED DELINEATORS ON ALL BARRIER INSTALLATIONS LOCATED ON A CURVED ALIGNMENT LONGER THAN 200 FEET AND ON BARRIERS USED TO SEPARATE OPPOSING TRAFFIC.
- SEE SHEET D FOR HOW TO ANCHOR BARRIER. SEE SHEET E FOR WHEN TO ANCHOR BARRIER.
- 1" CHAMFER OPTIONAL.

f'c = 4,000 psi

CONCRETE BARRIER  
TEMPORARY PRECAST, 12'-6"

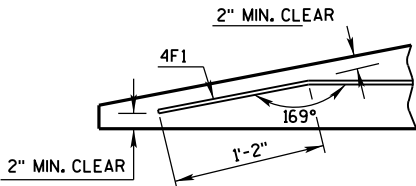
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



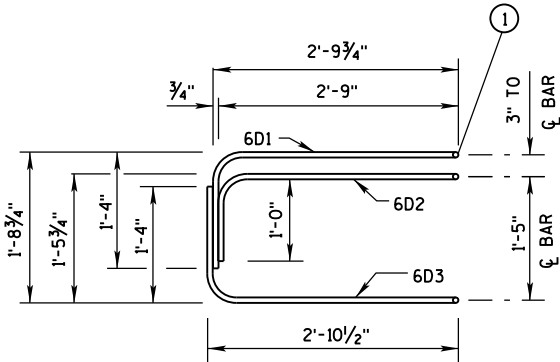
BARRIER TAPER SECTION  
BILL OF MATERIALS

(PER 12'-6" BARRIER TAPER SECTION)

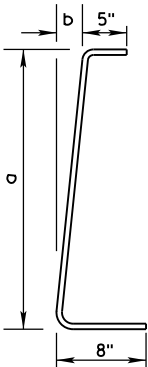
BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
4V1	4	2	1'-11"
4V2	4	2	2'-2"
4V3	4	2	2'-6"
4V4	4	2	2'-9"
4V5	4	2	3'-2"
4V6	4	2	3'-4"
4F1	4	2	12'-0"
4F2	4	2	7'-6"
5F3	5	1	11'-9"
LOOP ASSEMBLY			
6D1	6	1	8'-5"
6D2	6	1	7'-7"
6D3	6	1	8'-6"



DETAIL "C"  
BENT BAR DETAIL



ELEVATION  
LOOP BAR ASSEMBLY



4V BARS  
2 AT EACH SIZE REQUIRED  
FOR STIRRUP ASSEMBLY

BAR	a	b
V1	10"	1"
V2	1'-1"	1 1/4"
V3	1'-5"	1 5/8"
V4	1'-8"	1 7/8"
V5	2'-0 1/2"	2 3/8"
V6	2'-3"	2 3/4"

TAPER BARRIER SECTION

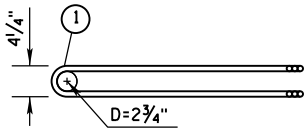
GENERAL NOTES

① NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.

BARRIER SECTION  
BILL OF MATERIALS

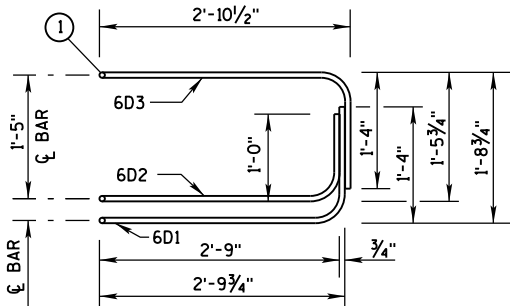
(PER 12'-6" BARRIER SECTION)

BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
4A1	4	12	6'-0"
6A2	6	6	2'-11"
5B1	5	3	12'-2"
4C1	4	2	12'-2"
LOOP ASSEMBLY			
6D1	6	2	8'-5"
6D2	6	2	7'-7"
6D3	6	2	8'-6"

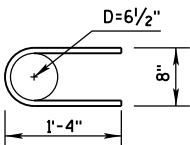


PLAN VIEW  
LOOP BAR ASSEMBLY

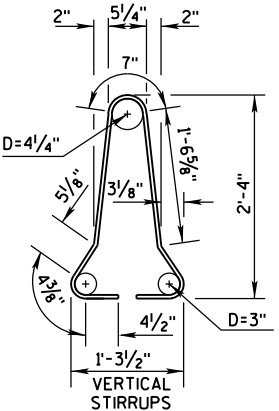
(MARKED END SHOWN, INVERT FOR OTHER END)



ELEVATION VIEW



6A2

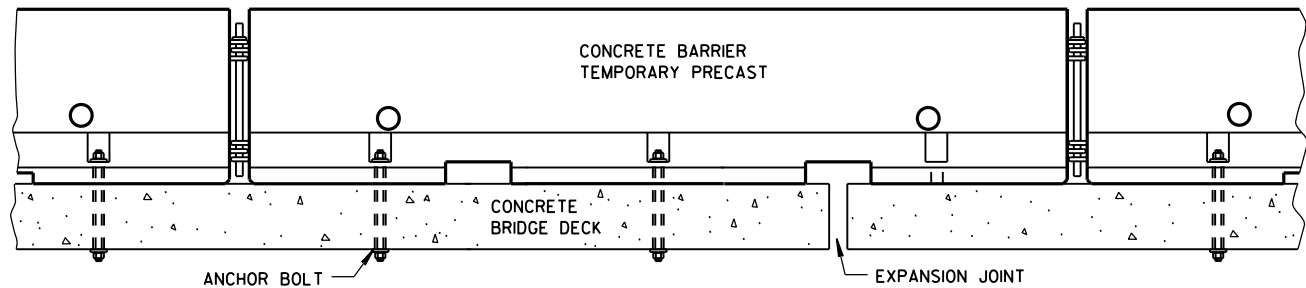
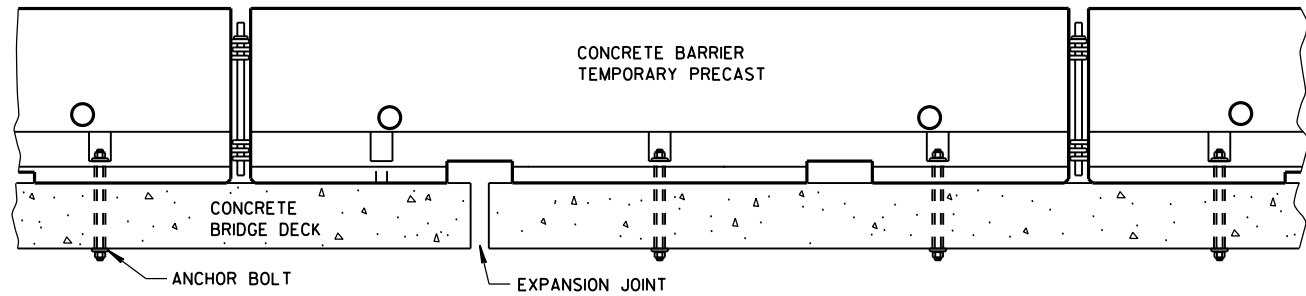


4A1

BARRIER SECTION

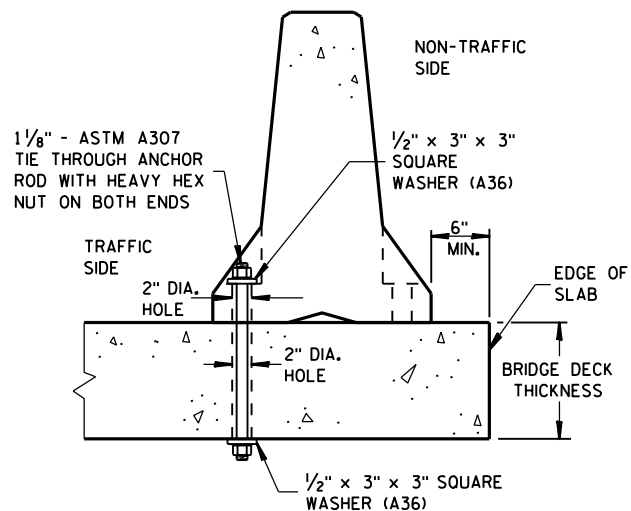
CONCRETE BARRIER  
TEMPORARY PRECAST, 12'-6"

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



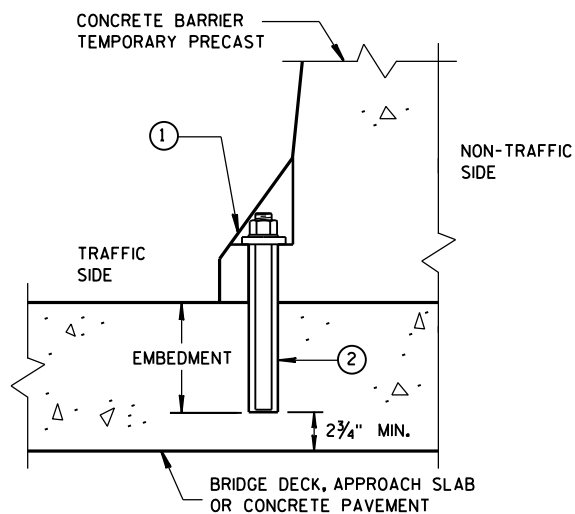
### TREATMENT AT BRIDGE DECK EXPANSION JOINTS

(NO SINGLE CONCRETE BARRIER SECTION SHALL BE ANCHORED TO BOTH THE BRIDGE DECK AND THE APPROACH SLAB. ALL ANCHOR BOLT LOCATIONS SHALL BE ANCHORED TO THE DECK IN ACCORDANCE WITH THE DETAIL. NO MORE THAN ONE ANCHOR BOLT SHALL BE ELIMINATED FROM A BARRIER SECTION WHEN SPANNING AN EXPANSION JOINT.)



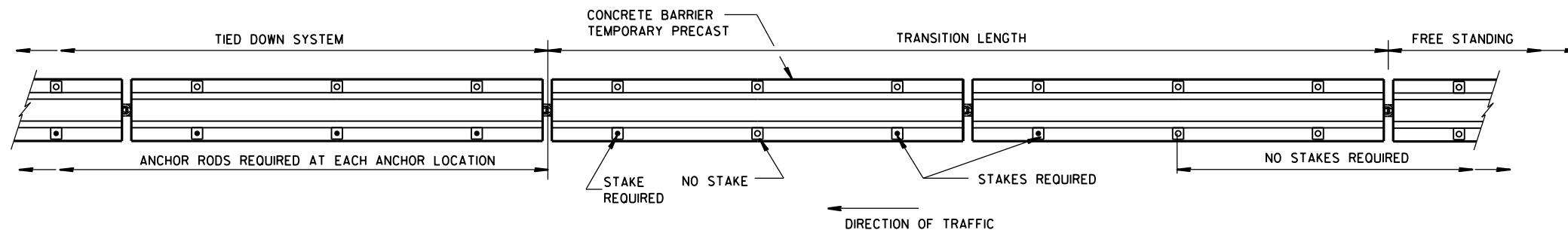
### THROUGH BOLTED ANCHOR INSTALLATION ON BRIDGE DECK

(DO NOT USE ON CONCRETE BRIDGE DECK WITH ASPHALT OVERLAY)



### REMOVABLE ADHESIVE ANCHOR INSTALLATION ON CONCRETE BRIDGE DECK, CONCRETE APPROACH SLAB, OR CONCRETE PAVEMENT

(DO NOT USE ON CONCRETE WITH AN ASPHALTIC OVERLAY)



### PLAN VIEW

### FREE STANDING TRANSITION TO TIED-DOWN SYSTEM

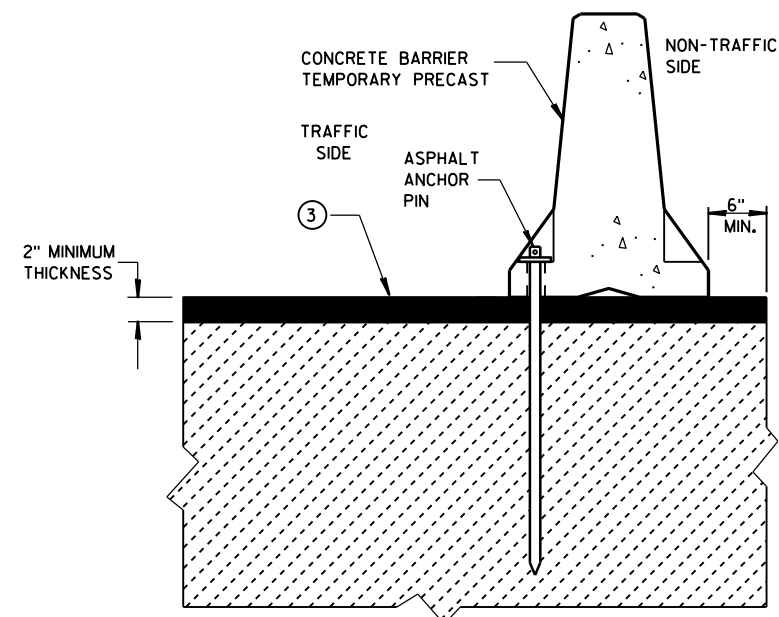
(PLACE TRANSITION IN A TANGENT SECTION OF BARRIER PARALLEL TO THE ROADWAY. IF TRANSITION OCCURS ON STRUCTURAL SLAB, ANCHOR AS SHOWN.)

### GENERAL NOTES

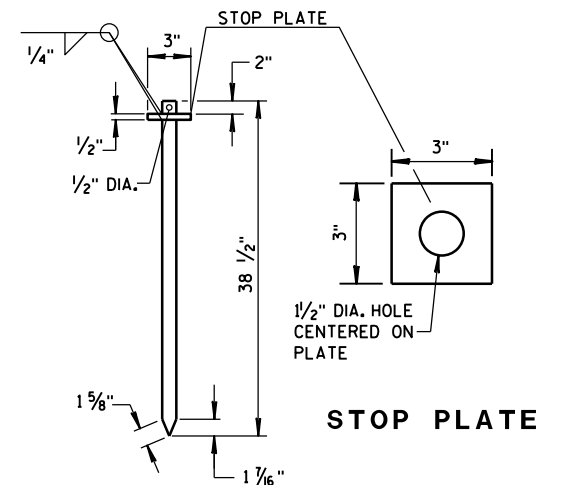
SEE SHEET E FOR WHEN TO ANCHOR. OTHER PARTS OF THE PLAN MAY SHOW ADDITIONAL LOCATIONS REQUIRING ANCHORING.

REMOVE ALL ANCHORS WHEN NO LONGER NEEDED. FILL CONCRETE PAVEMENTS, DECKS AND APPROACH SLABS WITH NON-SHRINK COMMERCIAL GROUT FROM THE APPROVED PRODUCT LIST. FILL ASPHALT PAVEMENTS WITH ASTM D6690 TYPE II RUBBERIZED CRACK FILLER.

- ① 1/8" DIAMETER A307 THREADED ROD, 1/2" X 3" X 3" SQUARE PLATE WASHER WITH ASTM A36 STEEL, ASTM A563A HEAVY HEX NUT.
- ② ADHESIVE ANCHORS WITH A MINIMUM BOND STRENGTH OF 1,800 PSI AND 5/4" EMBEDMENT. SEE 603.2 AND 603.3.12 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.
- ③ ASPHALT SURFACE SHOWN. CONTRACTOR MAY DRILL THROUGH CONCRETE PAVEMENT AND THEN DRIVE ASPHALT ANCHOR PIN.



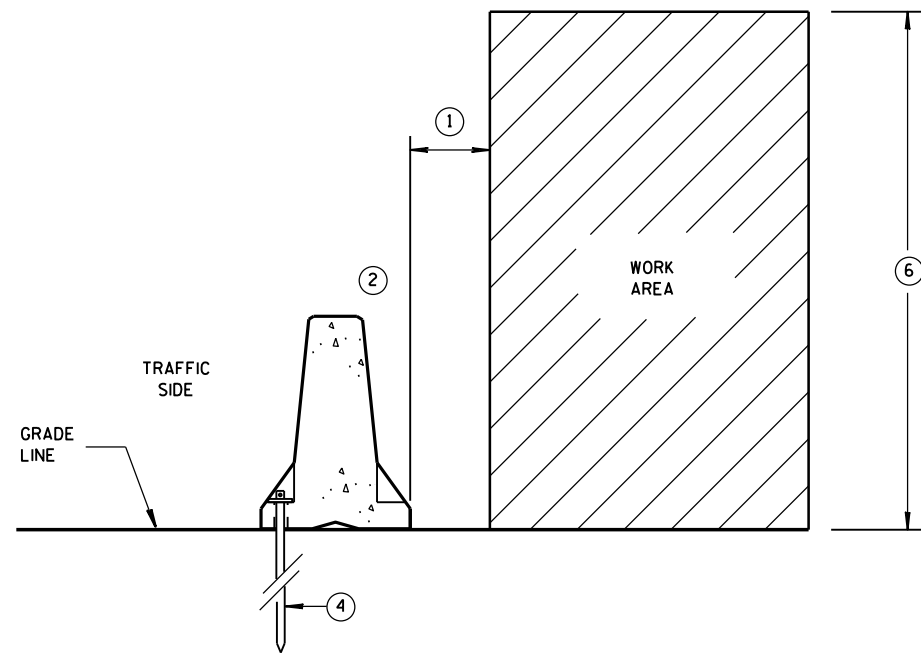
### STAKE DOWN INSTALLATION FOR ASPHALTIC SURFACE



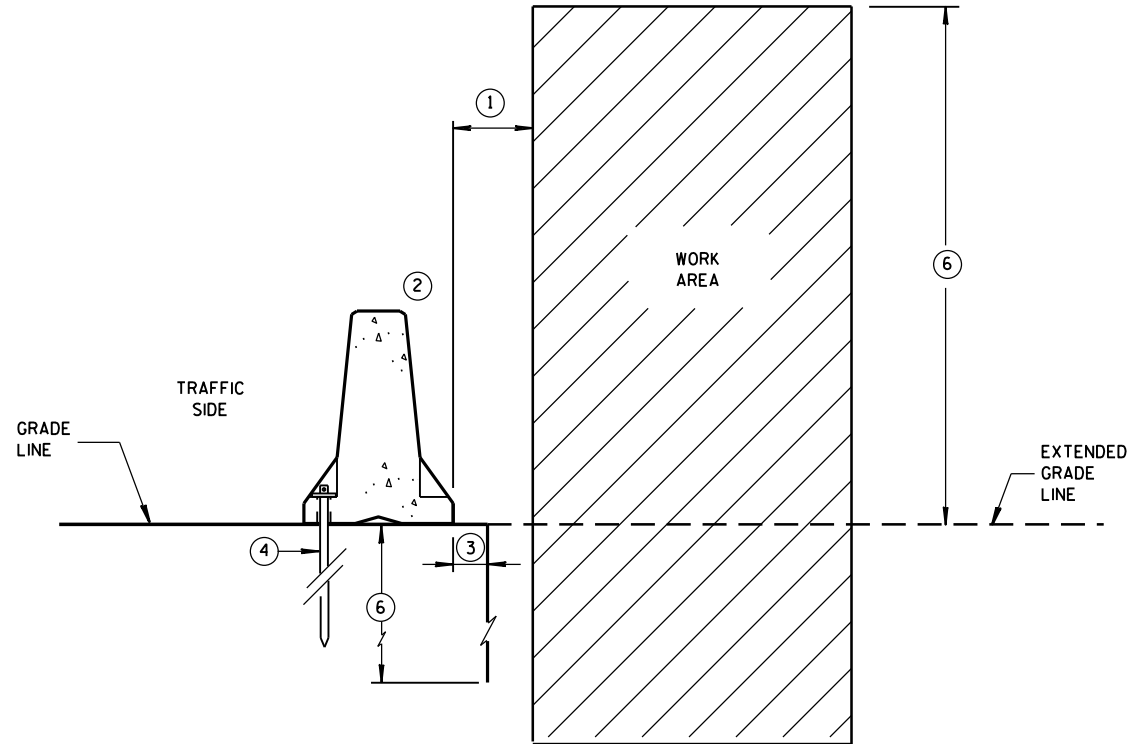
### ASPHALT ANCHOR PIN (ASTM A36 STEEL)

CONCRETE BARRIER  
TEMPORARY PRECAST, 12'-6"

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

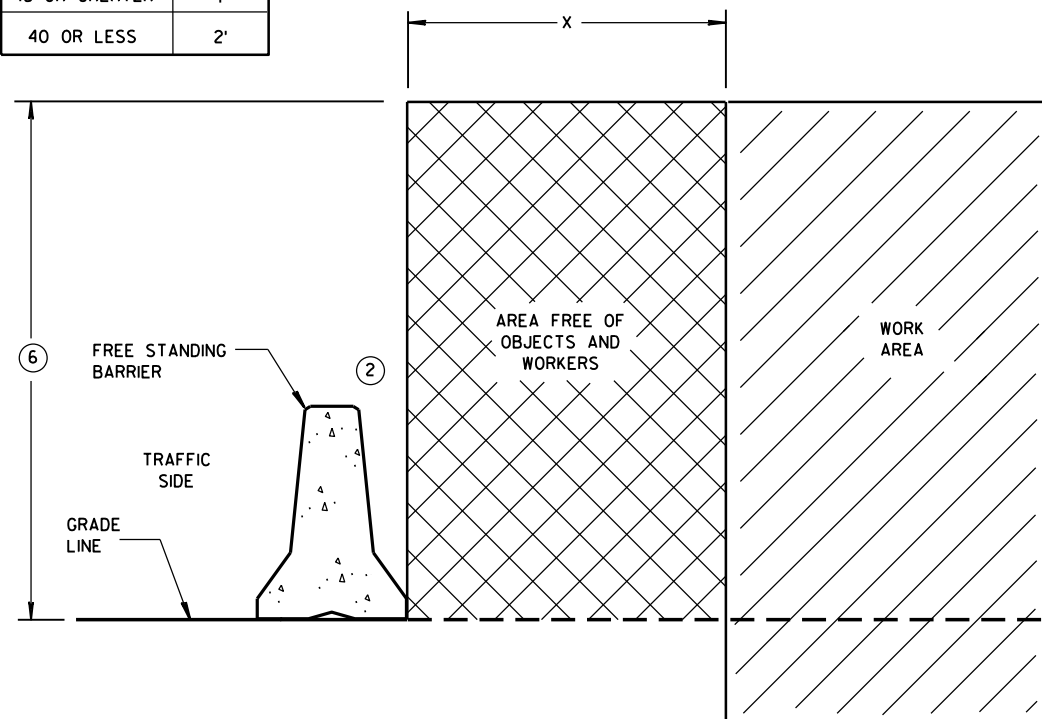


**ANCHORED BARRIER SPACE REQUIREMENTS  
FOR HAZARDS EXTENDED  
ABOVE THE GRADE LINE**

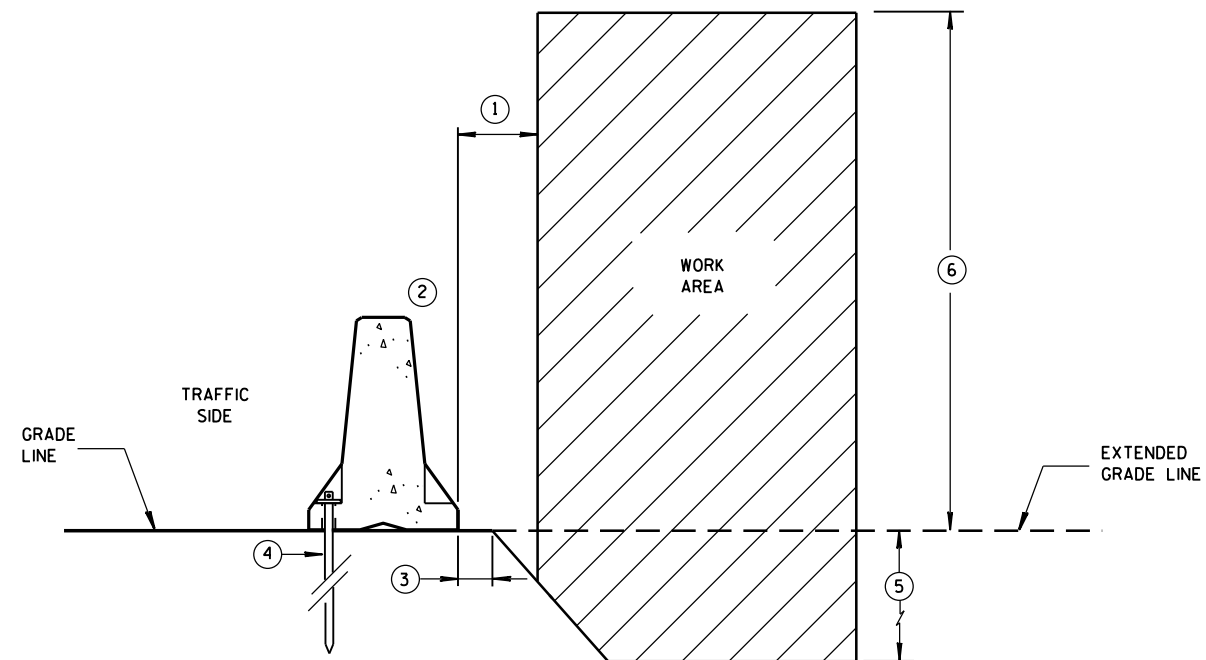


**ANCHORED BARRIER SPACE REQUIREMENTS  
ON VERTICAL DROP OFFS**

POSTED SPEED MPH	X
45 OR GREATER	4'
40 OR LESS	2'



**FREE STANDING BARRIER SPACE REQUIREMENTS**



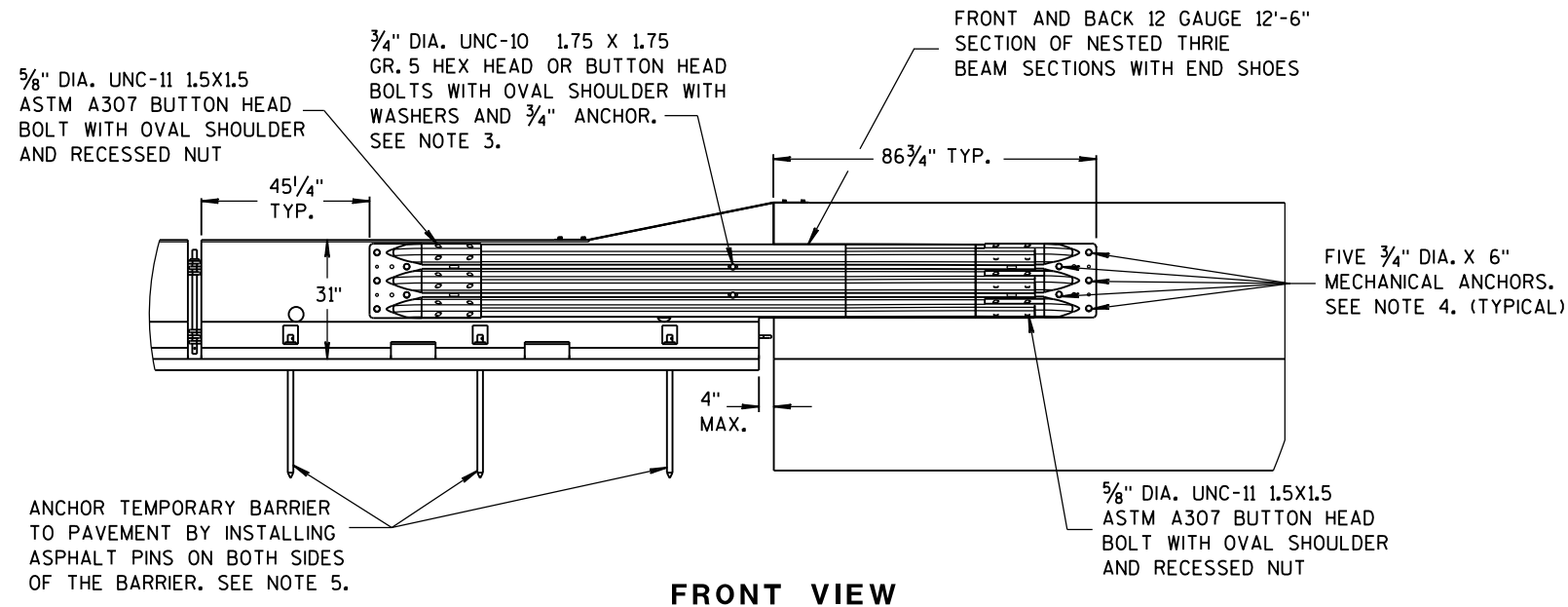
**ANCHORED BARRIER SPACE REQUIREMENTS  
ON SLOPES**

**GENERAL NOTES**

- 1 WHEN OBJECTS EXTEND ABOVE THE GRADE, A MINIMUM OF 1 FOOT IS REQUIRED FROM BACK OF BARRIER TO OBJECT. SEE OTHER DETAILS FOR FOR THE MINIMUM OFFSET FROM BACK OF BARRIER TO SLOPES OR VERTICAL DROPS.
- 2 OBJECTS ARE NOT TO BE PLACED ON, MOUNTED TO, OR LEANED AGAINST THE BARRIER WITHOUT PERMISSION OF THE PROJECT ENGINEER.
- 3 SEE OTHER DETAIL ON SHEET "D" FOR SPACE REQUIREMENTS.
- 4 SEE BOLT THROUGH DECK, REMOVABLE ADHESIVE ANCHOR, OR A STAKE DOWN FOR ASPHALTIC SURFACE TREATMENT DETAILS. ASPHALTIC ANCHOR SHOWN.
- 5 DEPTH OF 3 FEET OR MORE.
- 6 Y = 6'-6".

**CONCRETE BARRIER  
TEMPORARY PRECAST, 12'-6"**

**STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION**



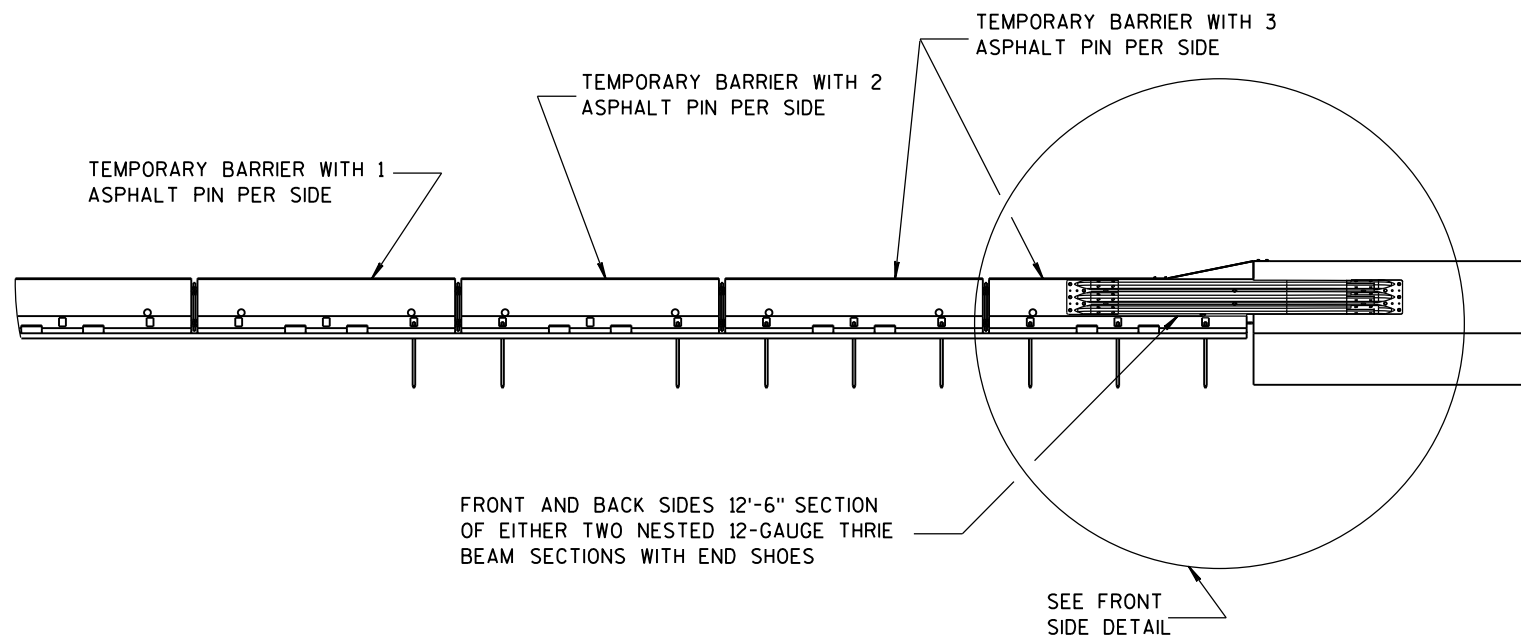
FRONT VIEW

# NOTES

NESTED THRIE BEAM IS REQUIRED ON BOTH SIDES OF THE TEMPORARY BARRIER FOR ALL INSTALLATIONS REGARDLESS OF TRAFFIC.

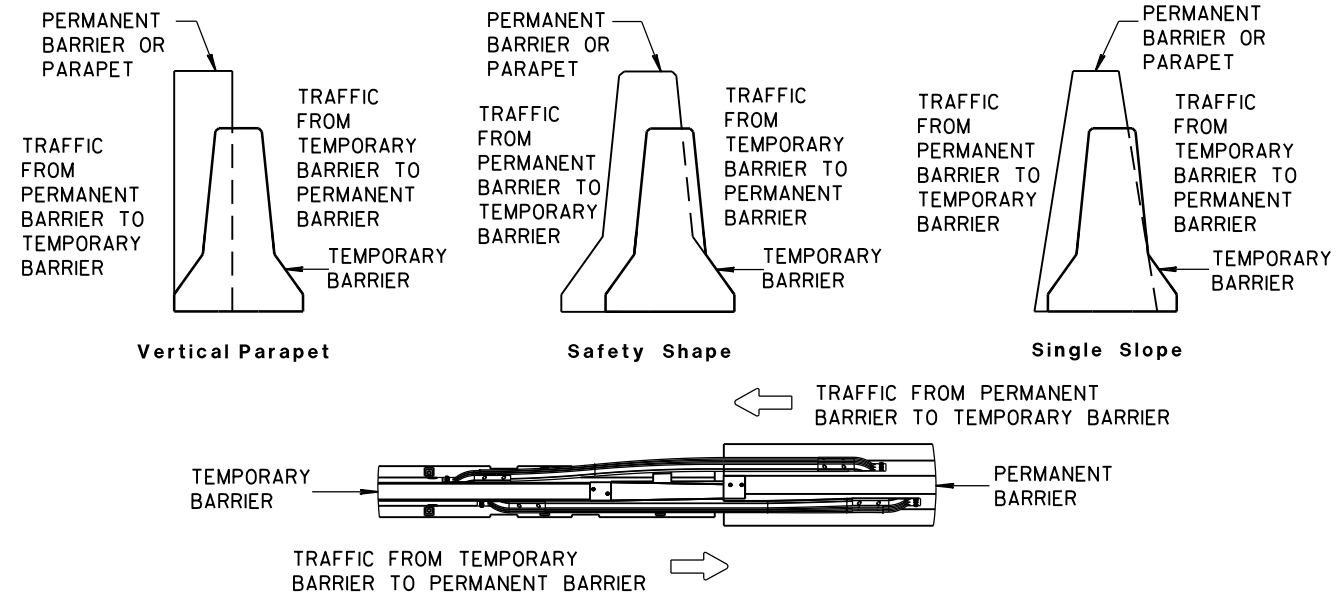
1. CAP END PLATE PLACED FLUSH WITH UPSTREAM END OF PERMANENT BARRIER OR PARAPET.
2. THRIE BEAM PIECES ARE OFFSET 15 1/4" TO PREVENT INTERFERENCE FROM THE ANCHORS ON OPPOSING SIDES.
3. MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 9.48 KIPS AND ULTIMATE SHEAR LOAD 10.48 KIPS.

4. MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 17.9 KIPS AND ULTIMATE SHEAR LOAD 21.96 KIPS.
5. MAY BE USED ON CONCRETE OR ASPHALT PAVEMENTS. ASPHALT OPTION SHOWN. FOR CONCRETE OPTION SEE OTHER DETAILS.
6. MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 12.14 KIPS AND ULTIMATE SHEAR LOAD 17.5 KIPS.

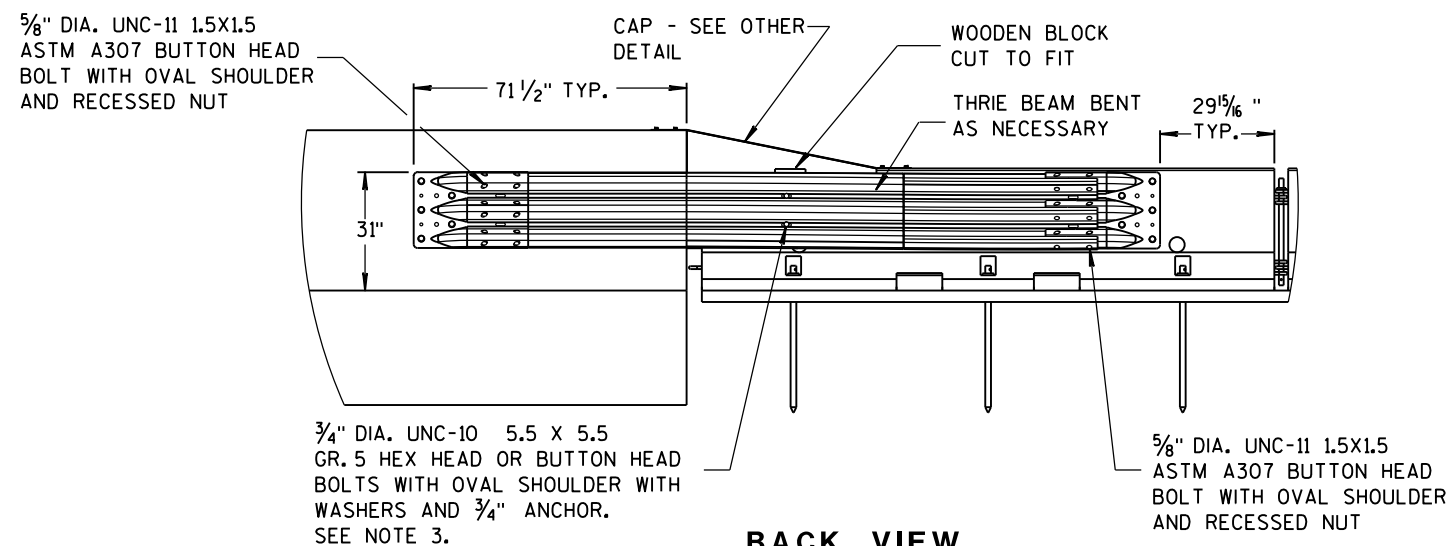


FRONT VIEW

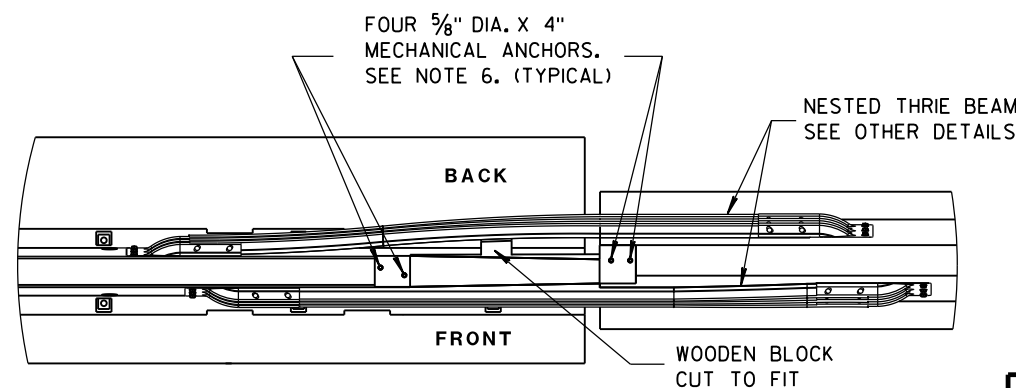
## BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM



## TEMPORARY BARRIER PLACEMENT FOR BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM



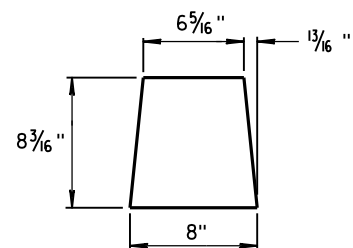
BACK VIEW



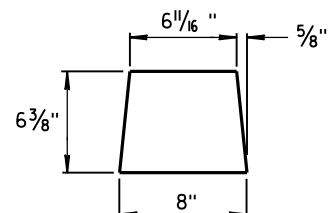
PLAN VIEW

CONCRETE BARRIER  
TEMPORARY PRECAST, 12'-6"

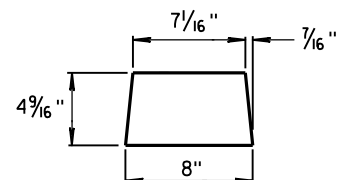
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



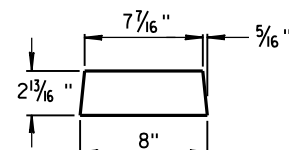
**GUSSET 1**



**GUSSET 2**

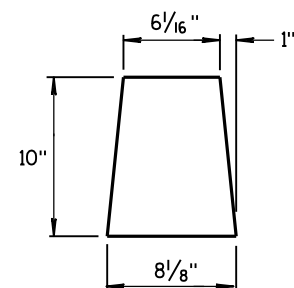


**GUSSET 3**

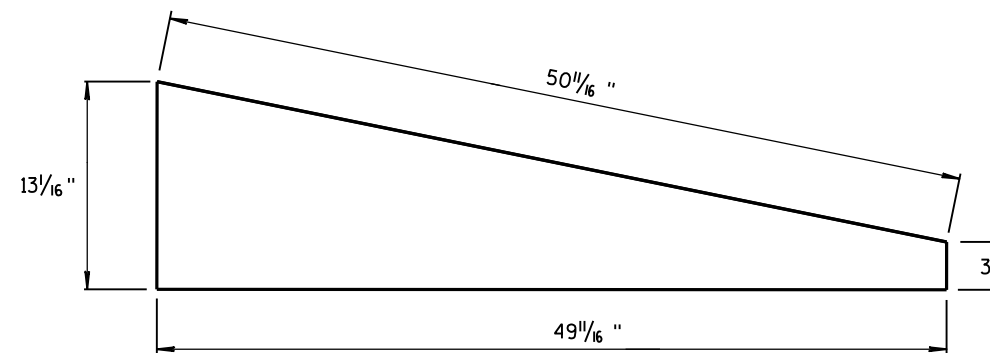


**GUSSET 4**

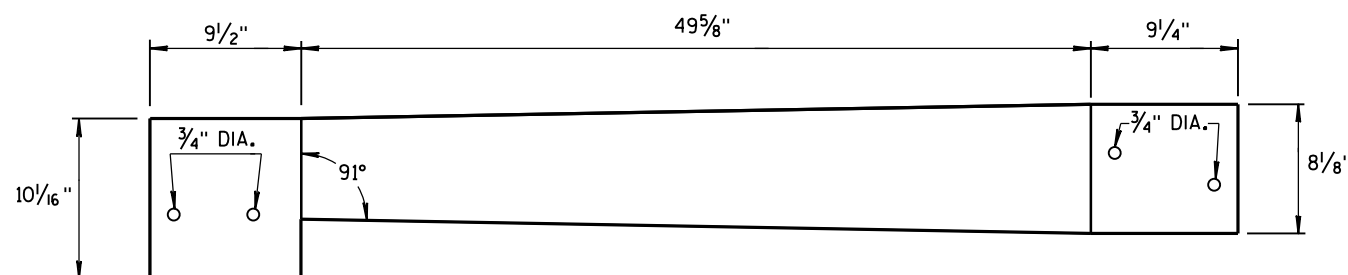
**GUSSETS**



**END PLATE**



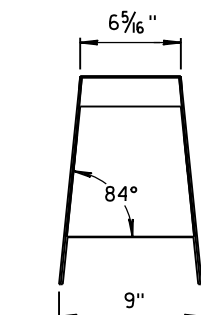
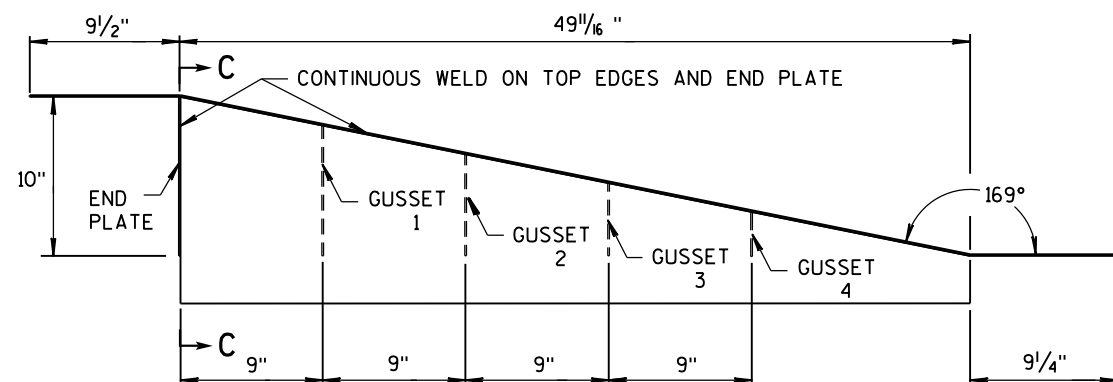
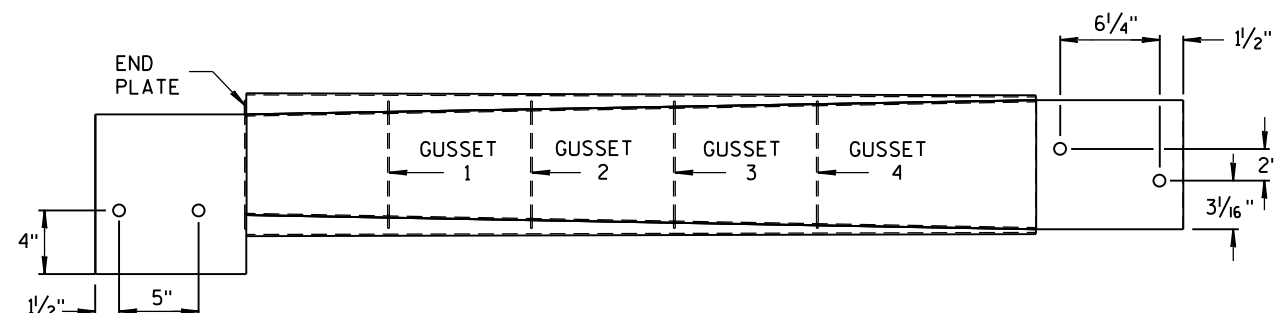
**SIDE PLATE**



**TOP PLATE**

**SIDE, TOP AND END PLATES FOR CAP  
FROM TEMPORARY CONCRETE BARRIER  
TO 42" PERMANENT CONCRETE BARRIER**

SIDE PLATES, TOP PLATE, END PLATE AND GUSSETS ARE 12 GAUGE ASTM A36 GALVANIZED STEEL.



**SECTION C-C**

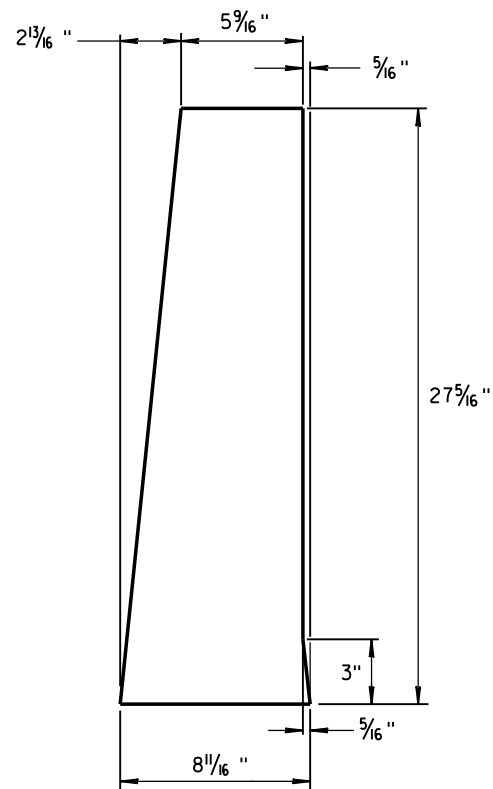
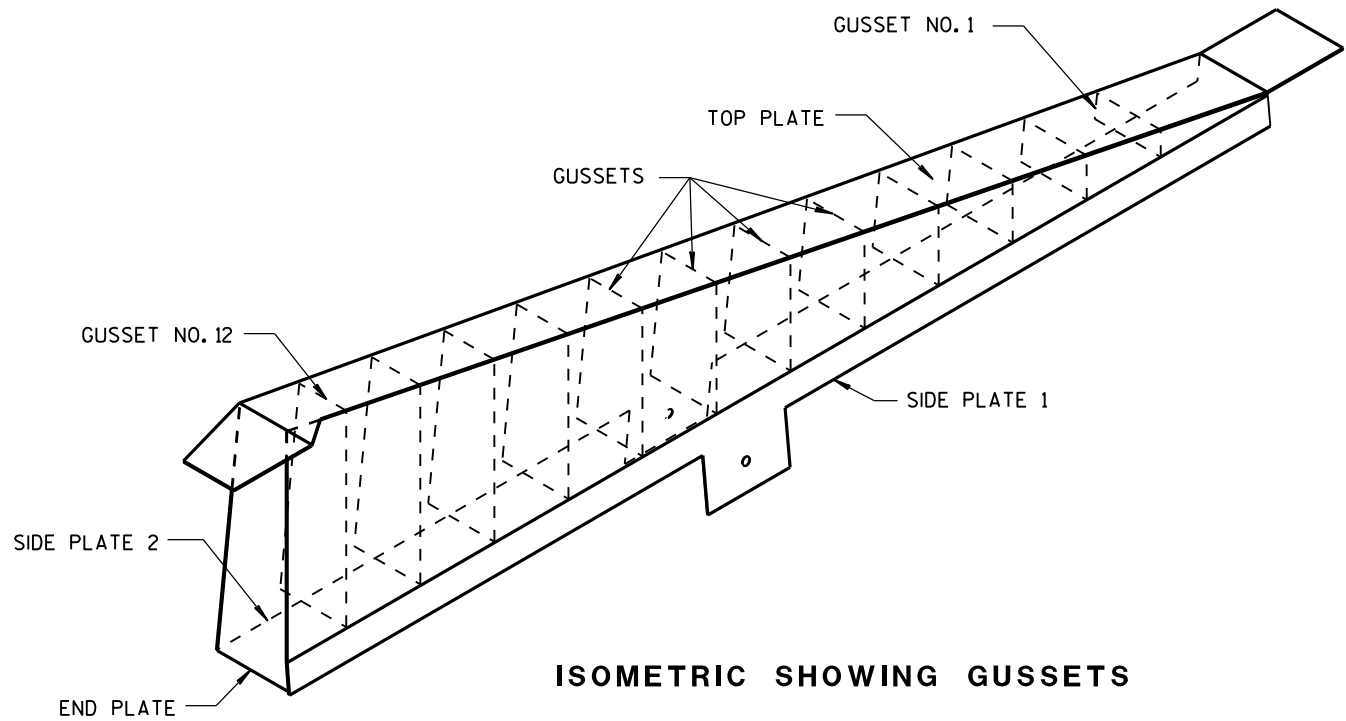
**NOTES**

1. FOUR GUSSETS AND END PLATE ARE STITCH WELDED ON THREE SIDES.
2. TWO TRIANGULAR SIDE PLATES ARE STITCH WELDED TO TOP PLATE, END PLATE, AND GUSSETS.

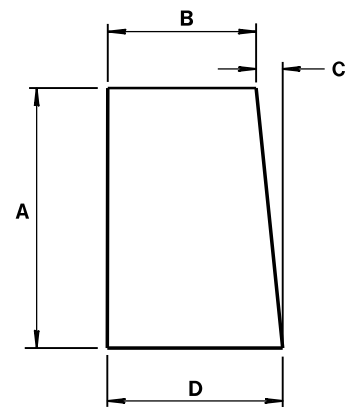
**CAP DETAILS FOR TEMPORARY CONCRETE  
BARRIER TO 42" PERMANENT CONCRETE BARRIER**

**CONCRETE BARRIER  
TEMPORARY PRECAST, 12'-6"**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



END PLATE  
1/8" STEEL PLATE

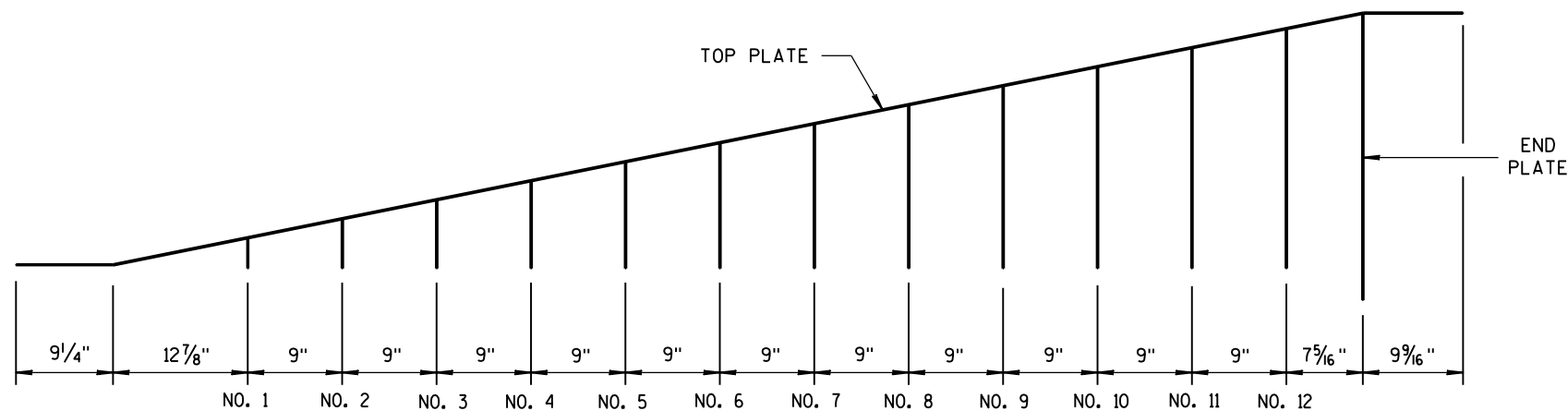


GUSSETS 1 - 12  
ALL GUSSETS 1/8" STEEL PLATE

GUSSET DIMENSIONS				
GUSSET NO.	A	B	C	D
1	2 7/8"	7 3/4"	1/4"	8
2	4 1/16 "	7 9/16 "	1/2"	8
3	6 1/2"	7 3/8 "	1 1/16 "	8 1/16 "
4	8 5/16 "	7 3/16 "	7/8"	8 1/16 "
5	10 1/8 "	7"	1 1/16 "	8 1/16 "
6	11 5/16 "	6 13/16 "	1 1/4"	8 1/16 "
7	13 3/4"	6 5/8 "	1 7/16 "	8 1/16 "
8	15 9/16 "	6 7/16 "	1 9/16 "	8 1/16 "
9	17 3/8"	6 1/4"	1 13/16 "	8 1/16 "
10	19 3/16 "	6 1/16 "	1 15/16 "	8 1/16 "
11	21"	5 7/8 "	2 3/16 "	8 1/16 "
12	22 13/16 "	5 11/16 "	2 5/16 "	8 1/16 "

SIDE PLATES, TOP PLATE, END PLATE AND GUSSETS ARE 12 GAUGE ASTM A36 STEEL AND GALVANIZED.

GUSSETS AND END PLATE ARE STITCH WELDED ON 3 SIDES. TWO TRIANGULAR SIDE PLATES ARE STITCH WELDED TO TOP PLATE, END PLATE AND GUSSETS.

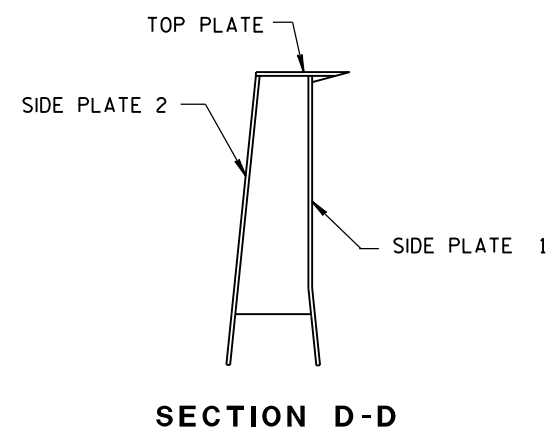
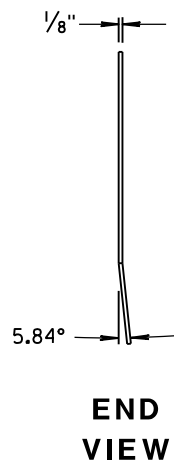
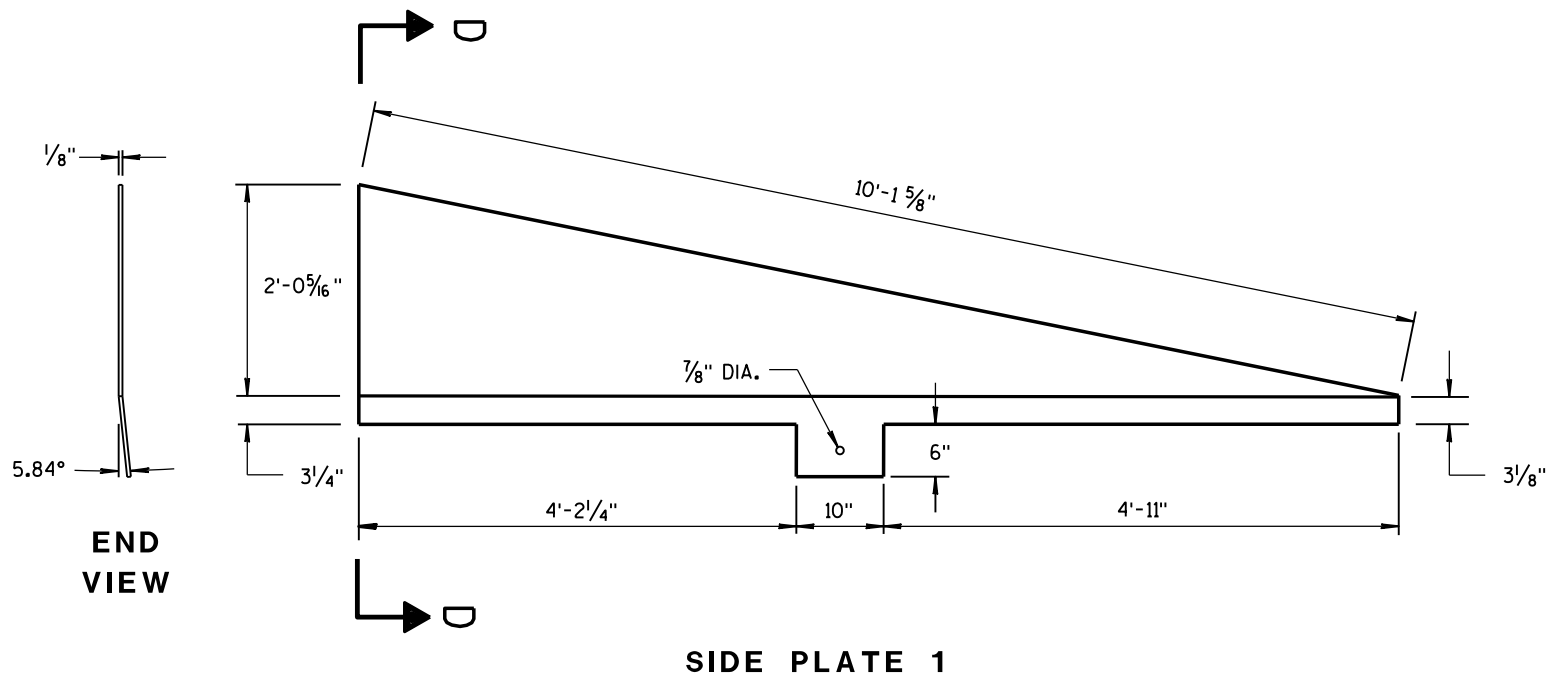
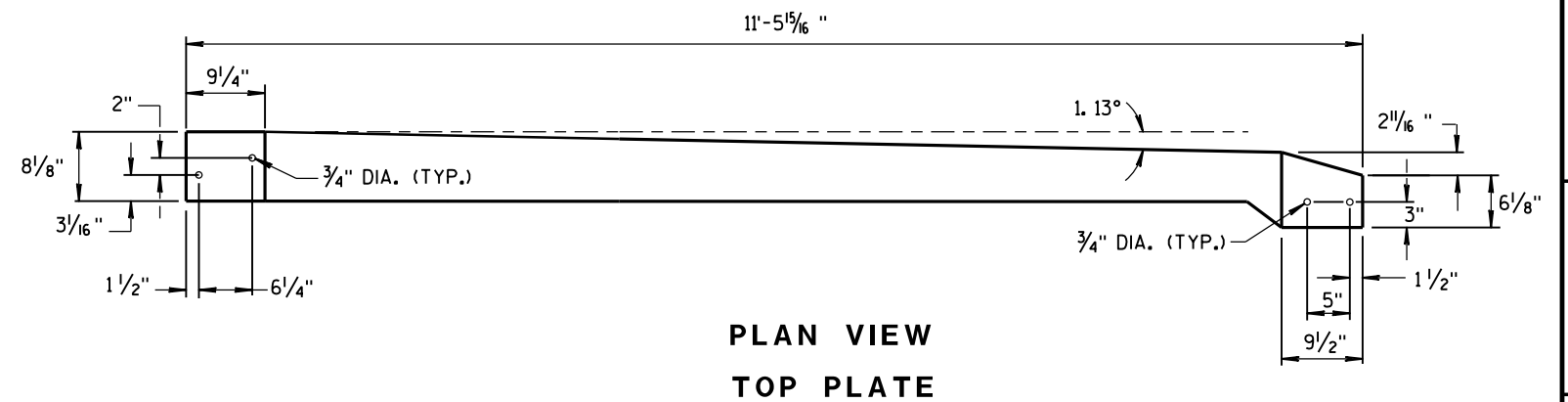
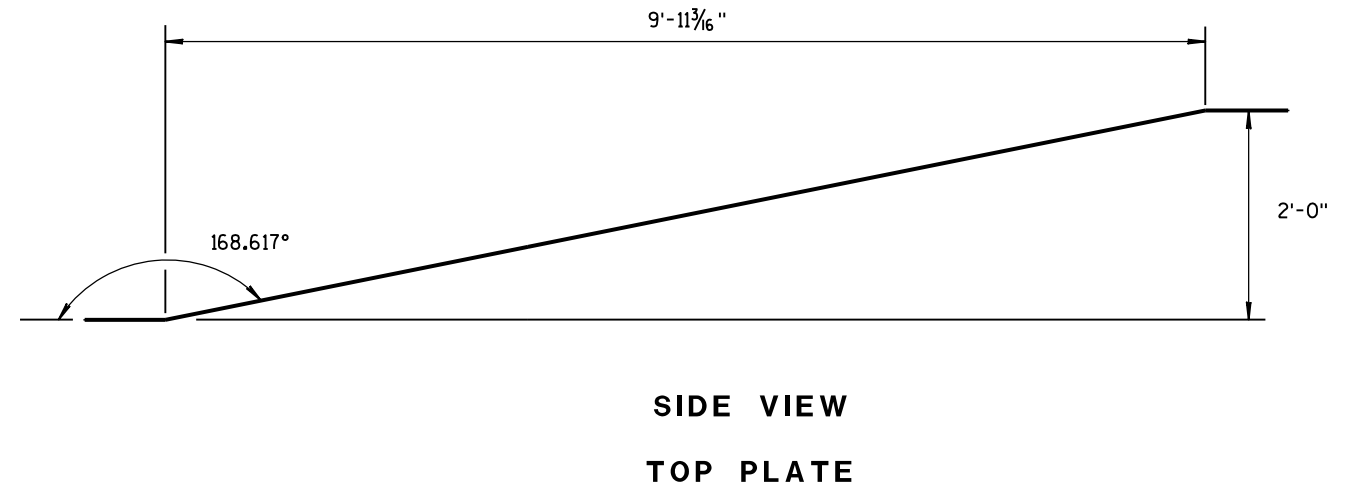
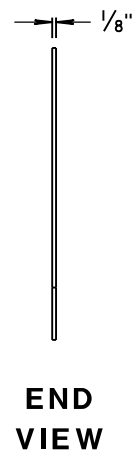
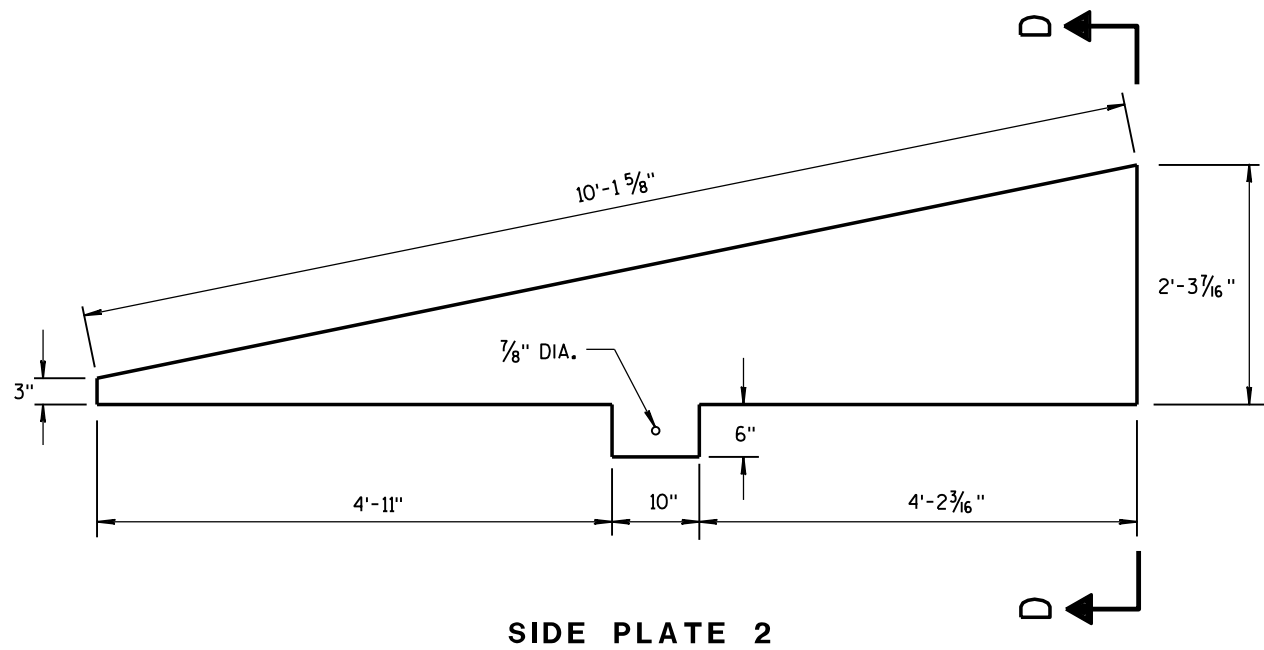


CAP DETAILS FOR TEMPORARY CONCRETE  
BARRIER TO 56" PERMANENT CONCRETE BARRIER

CONCRETE BARRIER  
TEMPORARY PRECAST, 12'-6"

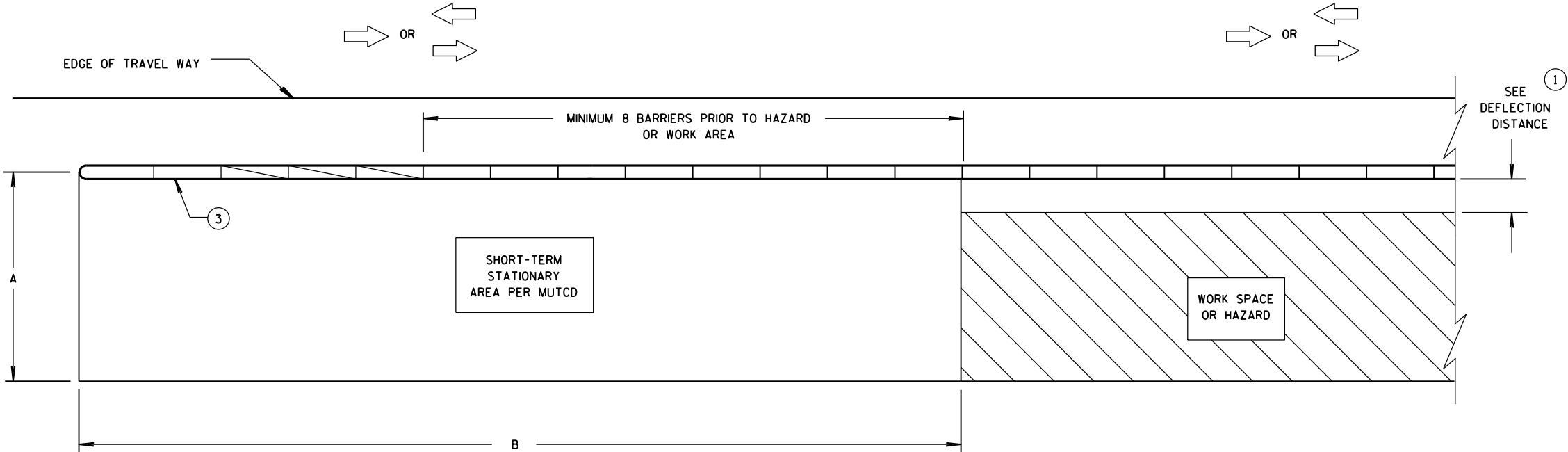
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



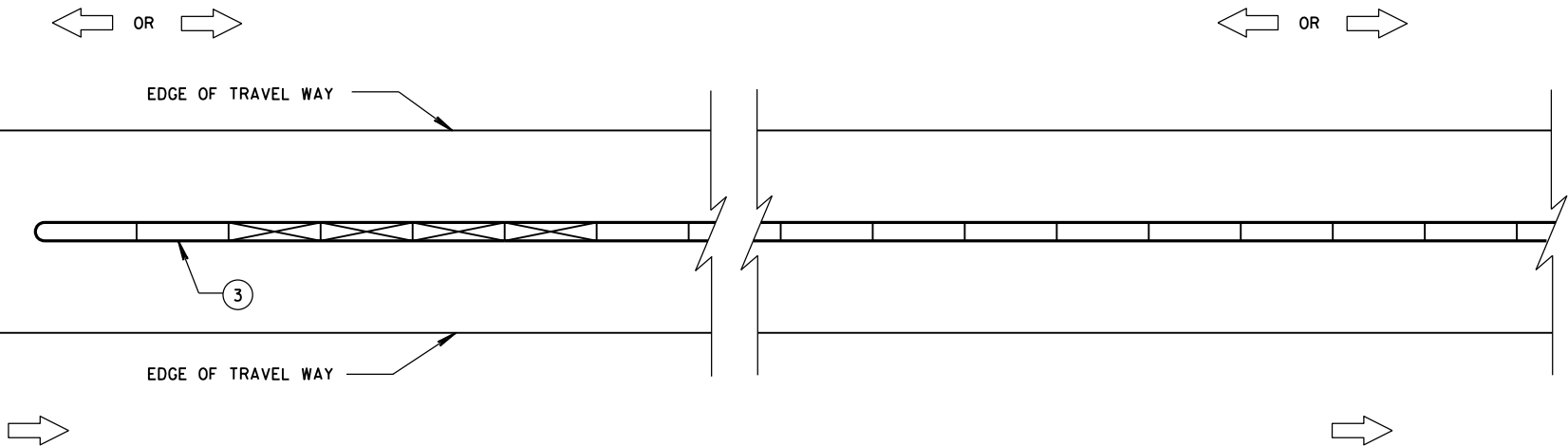


**CAP DETAILS FOR TEMPORARY CONCRETE  
BARRIER TO 56" PERMANENT CONCRETE BARRIER**

CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Rodney Taylor ROADWAY STANDARD DEVELOPMENT UNIT SUPERVISOR
FHWA	



**CRASH CUSHION/SAND BARREL ARRAY AND TEMPORARY BARRIER  
INSTALLATION FOR TRAFFIC ON ONE SIDE OF BARRIER**



**CRASH CUSHION/SAND BARREL ARRAY AND TEMPORARY BARRIER  
INSTALLATION FOR TRAFFIC ON BOTH SIDES OF BARRIER**

**GENERAL NOTES**

SEE STANDARD DETAIL DRAWING 14B7 FOR MORE INFORMATION.

DETAILS PROVIDE A GENERAL LAYOUT OF TEMPORARY CONCRETE BARRIER, CRASH CUSHIONS, SAND BARREL ARRAYS AND TIE DOWN TRANSITIONS. DETAILS PROVIDED MAY NOT FIT ALL POSSIBLE SITUATIONS OR SITE CONDITIONS. SEE OTHER SECTIONS OF THE CONTRACT OR PROJECT ENGINEER FOR MORE DETAILS.

ADDITIONAL TEMPORARY BARRIER MAY BE REQUIRED TO PROTECT TRAVELING PUBLIC FROM HAZARDS, CONTRACTOR'S OPERATIONS OR TO CONTROL TRAFFIC.

TEMPORARY BARRIER MAY BE REQUIRED TO BE ANCHORED TO PAVEMENT OR BRIDGE DECK.

FOR DETAILS ON CRASH CUSHION OR SAND BARREL ARRAYS SEE OTHER SECTIONS OF THE PLAN AND MANUFACTURE'S DETAILS.

SLOPES LEADING TO TEMPORARY BARRIER, CRASH CUSHION OR SAND BARREL ARRAY ARE 10:1 OR LESS.

① FOR DEFLECTION INFORMATION SEE STANDARD DETAIL DRAWING 14B7.

② VALUES PROVIDED MAY NOT FIT ALL POSSIBLE SITUATIONS OR SITE CONDITIONS. SEE OTHER SECTIONS OF THE CONTRACT OR PROJECT ENGINEER FOR MORE DETAILS.

③ ANCHOR TEMPORARY BARRIER ACCORDING TO CRASH CUSHION OR SAND BARREL MANUFACTURER'S RECOMMENDATIONS. IF MANUFACTURER'S RECOMMENDATIONS ARE NOT PROVIDED, ANCHOR 3 PINS ON TRAFFIC SIDE.

**DIMENSION A TABLE ②**

FACILITY	POSTED SPEED MPH	DIMENSION A	
		MIN. FT	MAX. FT
FREEWAY/EXPRESSWAY	ALL	15	20
NON-FREEWAY/EXPRESSWAY	GREATER THAN OR EQUAL TO 45	10	15
NON-FREEWAY/EXPRESSWAY	LESS THAN 45	8	10
AADT LESS THAN 1,500	ALL	8	10

**DIMENSION B TABLE ②**

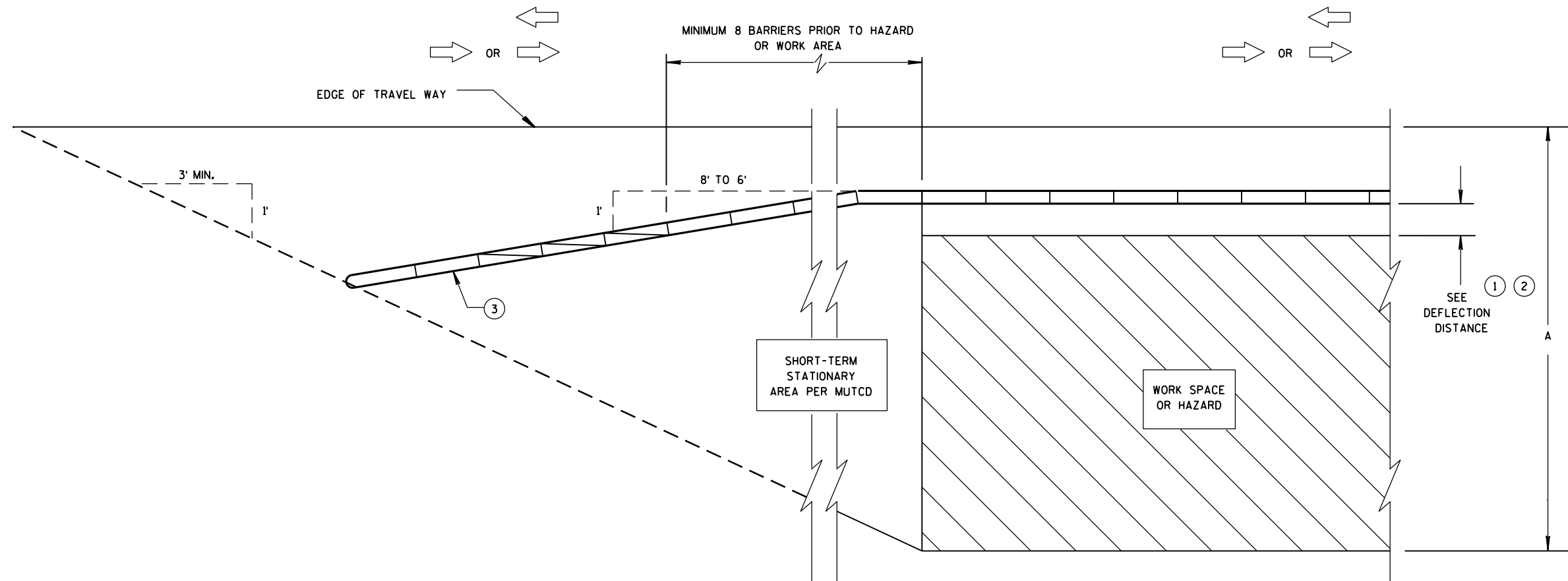
POSTED SPEEDS MPH	DIMENSION B FT
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645

**LEGEND**

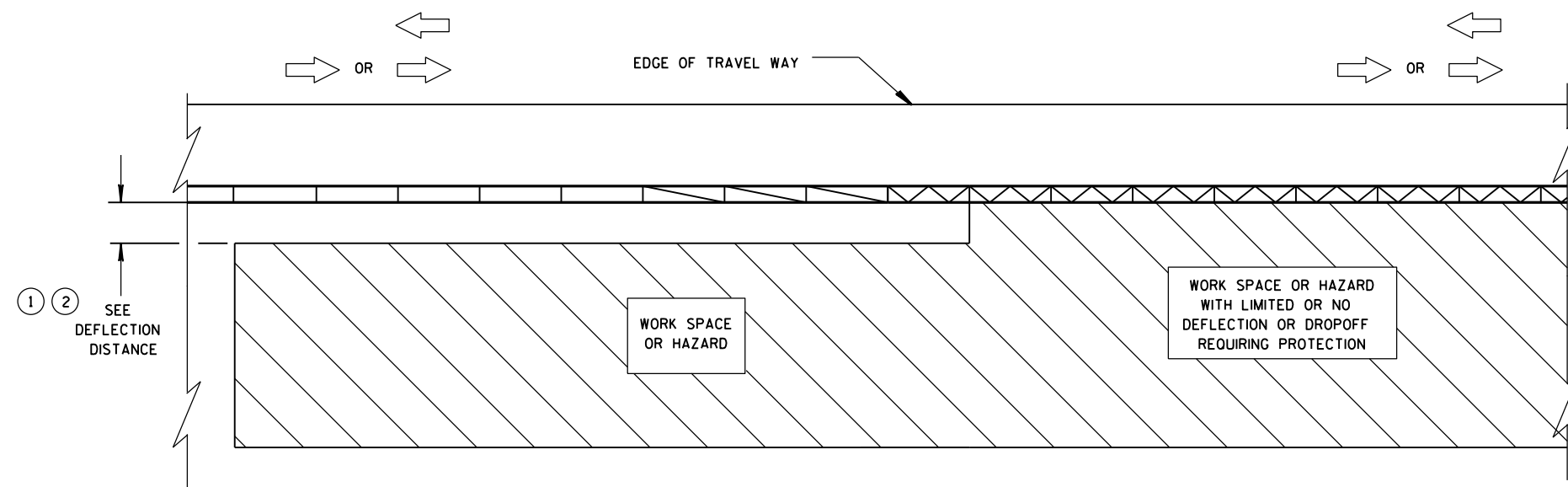
- DIRECTION OF TRAVEL →
- CRASH CUSHION OR SAND BARREL ARRAY
- SEE FREE STANDING TRANSITION TO TIED-DOWN SYSTEM DETAILS
- SEE BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM DETAILS
- 3 PINS PLACED ON TRAFFIC SIDE OF BARRIER
- PERMANENT CONCRETE BARRIER OR CONCRETE PARAPET
- FREE STANDING TEMPORARY BARRIER

**CRASH CUSHION/SAND BARREL  
ARRAY AND OTHER TEMPORARY  
BARRIER LAYOUT DETAILS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**CRASH CUSHION/SAND BARREL ARRAY AND TEMPORARY BARRIER  
INSTALLATION FOR TRAFFIC ON ONE SIDE - FLARED INSTALLATION**



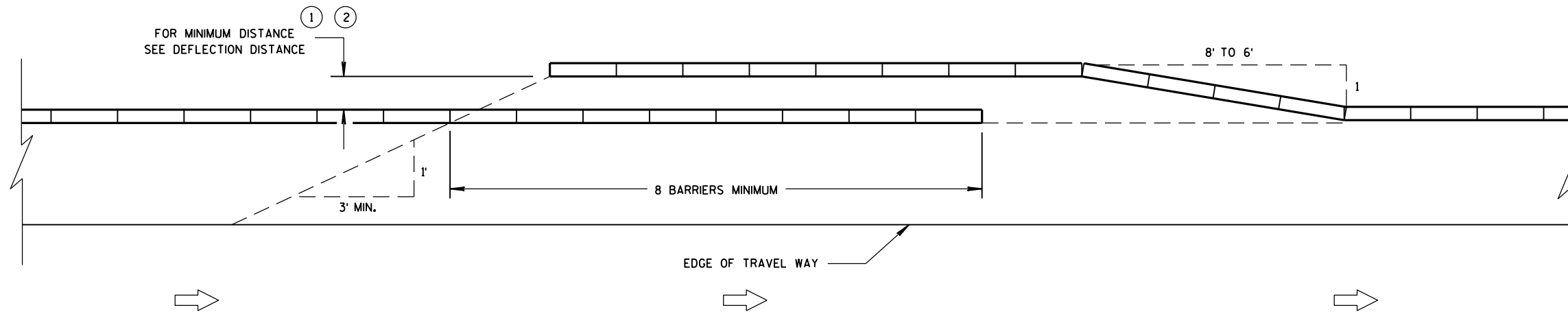
**TRANSITION FROM FREE STANDING TEMPORARY BARRIER  
TO ANCHORED BARRIER**

**LEGEND**

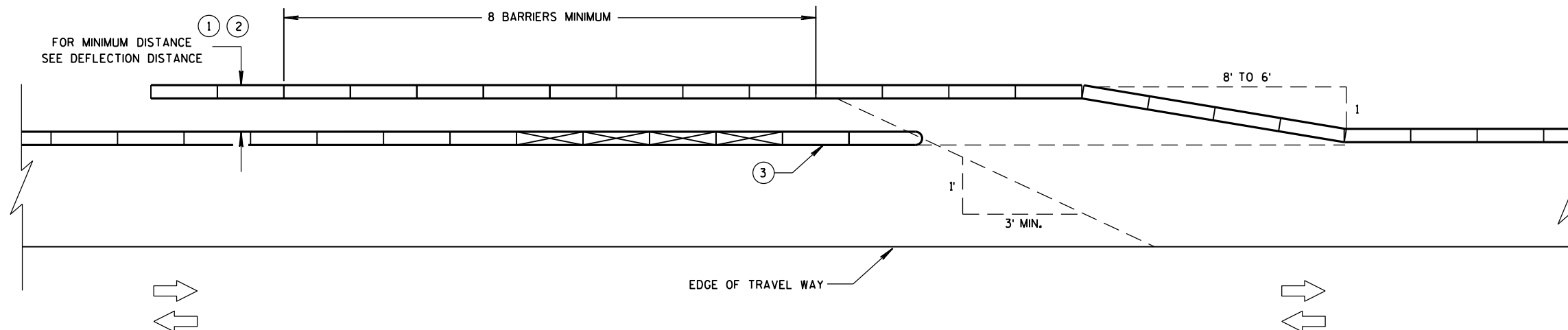
DIRECTION OF TRAVEL	
CRASH CUSHION OR SAND BARREL ARRAY	
SEE FREE STANDING TRANSITION TO TIED-DOWN SYSTEM DETAILS	
SEE BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM DETAILS	
3 PINS PLACED ON TRAFFIC SIDE OF BARRIER	
PERMANENT CONCRETE BARRIER OR CONCRETE PARAPET	
FREE STANDING TEMPORARY BARRIER	

**CRASH CUSHION/SAND BARREL  
ARRAY AND OTHER TEMPORARY  
BARRIER LAYOUT DETAILS**

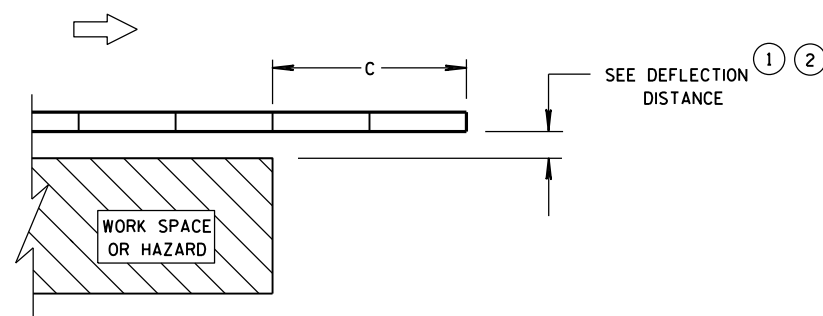
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



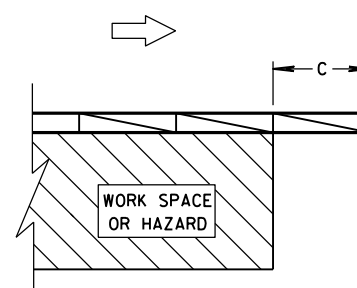
**TEMPORARY BARRIER OVERLAP - ONE-WAY TRAFFIC**



**TEMPORARY BARRIER OVERLAP - TWO-WAY TRAFFIC**



**ENDING TEMPORARY BARRIER  
DOWNSTREAM - UNANCHORED**



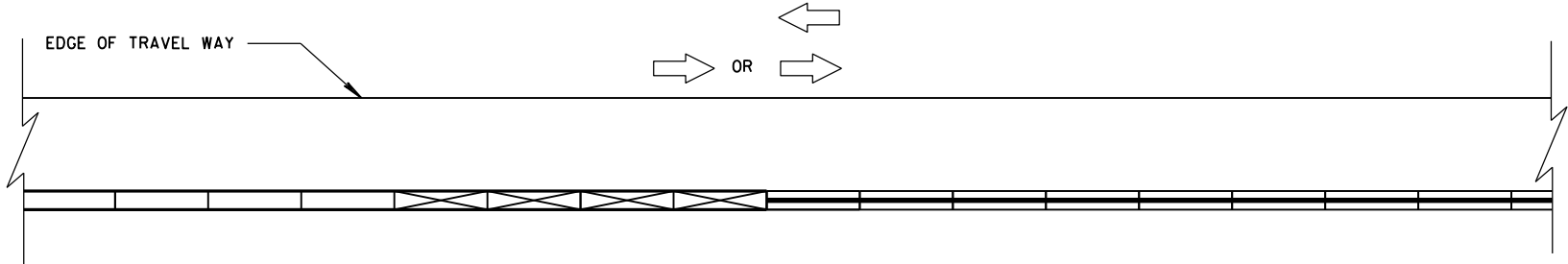
**ENDING TEMPORARY BARRIER  
DOWNSTREAM - ANCHORED**

**LEGEND**

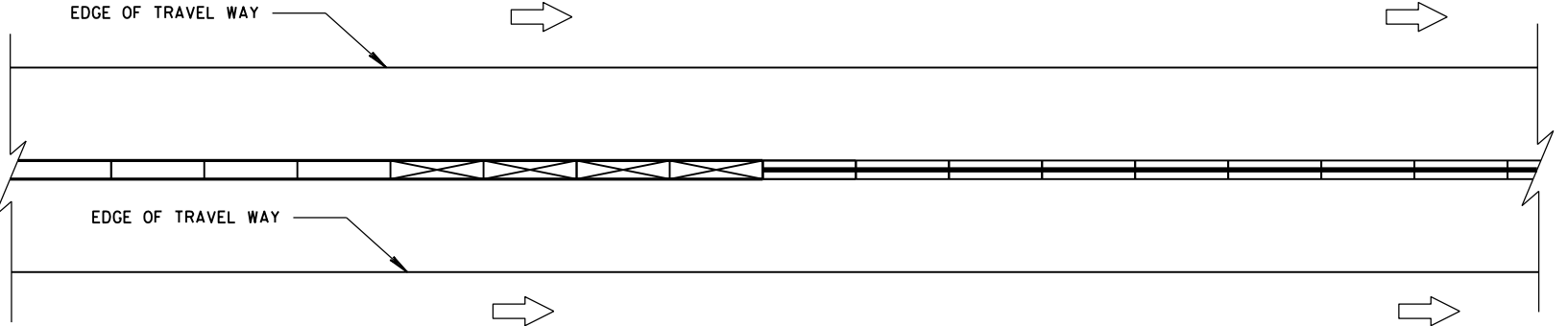
DIRECTION OF TRAVEL	
CRASH CUSHION OR SAND BARREL ARRAY	
SEE FREE STANDING TRANSITION TO TIED-DOWN SYSTEM DETAILS	
SEE BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM DETAILS	
3 PINS PLACED ON TRAFFIC SIDE OF BARRIER	
PERMANENT CONCRETE BARRIER OR CONCRETE PARAPET	
FREE STANDING TEMPORARY BARRIER	

**CRASH CUSHION/SAND BARREL  
ARRAY AND OTHER TEMPORARY  
BARRIER LAYOUT DETAILS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

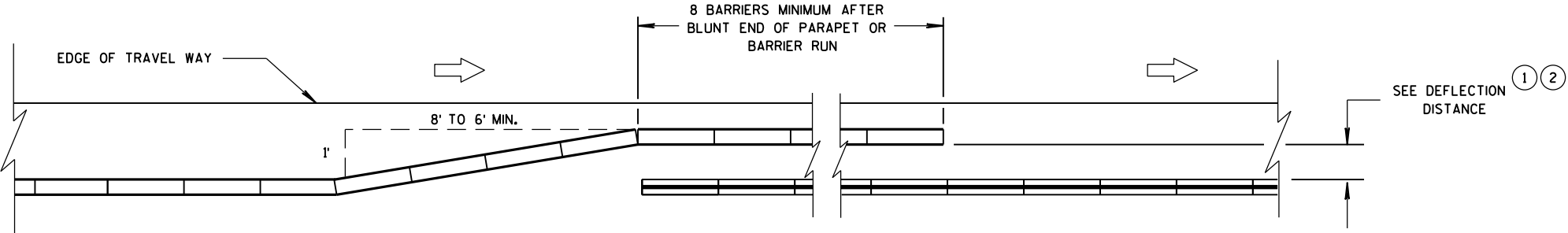


CONNECTING TEMPORARY BARRIER TO PERMANENT  
CONCRETE BARRIER-TRAFFIC ON ONE SIDE

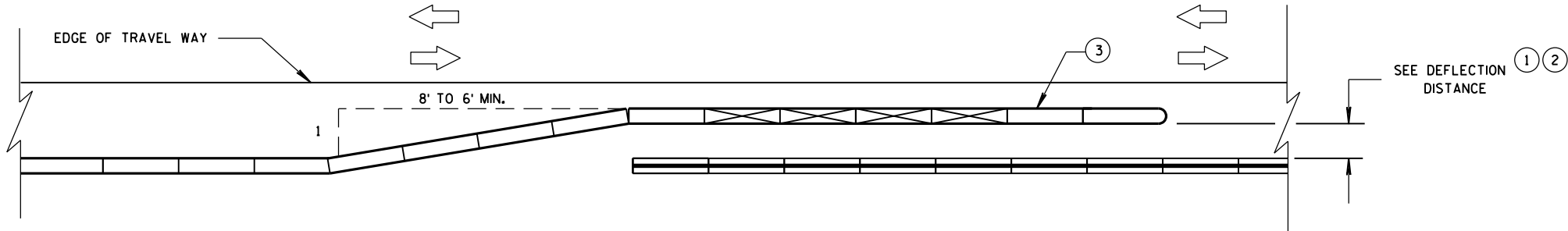


CONNECTING TEMPORARY BARRIER TO PERMANENT  
CONCRETE BARRIER-TRAFFIC ON BOTH SIDES

LEGEND	
DIRECTION OF TRAVEL	
CRASH CUSHION OR SAND BARREL ARRAY	
SEE FREE STANDING TRANSITION TO TIED-DOWN SYSTEM DETAILS	
SEE BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM DETAILS	
3 PINS PLACED ON TRAFFIC SIDE OF BARRIER	
PERMANENT CONCRETE BARRIER OR CONCRETE PARAPET	
FREE STANDING TEMPORARY BARRIER	



OVERLAPPING TEMPORARY BARRIER AND PERMANENT BARRIER -  
ONE WAY TRAFFIC



OVERLAPPING TEMPORARY BARRIER AND PERMANENT BARRIER -  
TWO WAY TRAFFIC

CRASH CUSHION/SAND BARREL  
ARRAY AND OTHER TEMPORARY  
BARRIER LAYOUT DETAILS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

LEGEND

- DIRECTION OF TRAVEL
- CRASH CUSHION OR SAND BARREL ARRAY
- SEE FREE STANDING TRANSITION TO TIED-DOWN SYSTEM DETAILS
- SEE BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM DETAILS
- 3 PINS PLACED ON TRAFFIC SIDE OF BARRIER
- PERMANENT CONCRETE BARRIER OR CONCRETE PARAPET
- FREE STANDING TEMPORARY BARRIER

DIMENSION C TABLE

2

AVAILABLE DEFLECTION DISTANCE	MINIMUM LENGTH OF BARRIER BEYOND HAZARD FT
GREATER THAN 8'	12.5
LESS THAN OR EQUAL TO 8' BUT GREATER THAN 4'	50
LESS THAN OR EQUAL TO 4'	100

6

6

CRASH CUSHION/SAND BARREL  
ARRAY AND OTHER TEMPORARY  
BARRIER LAYOUT DETAILS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

June, 2015

DATE

FHWA

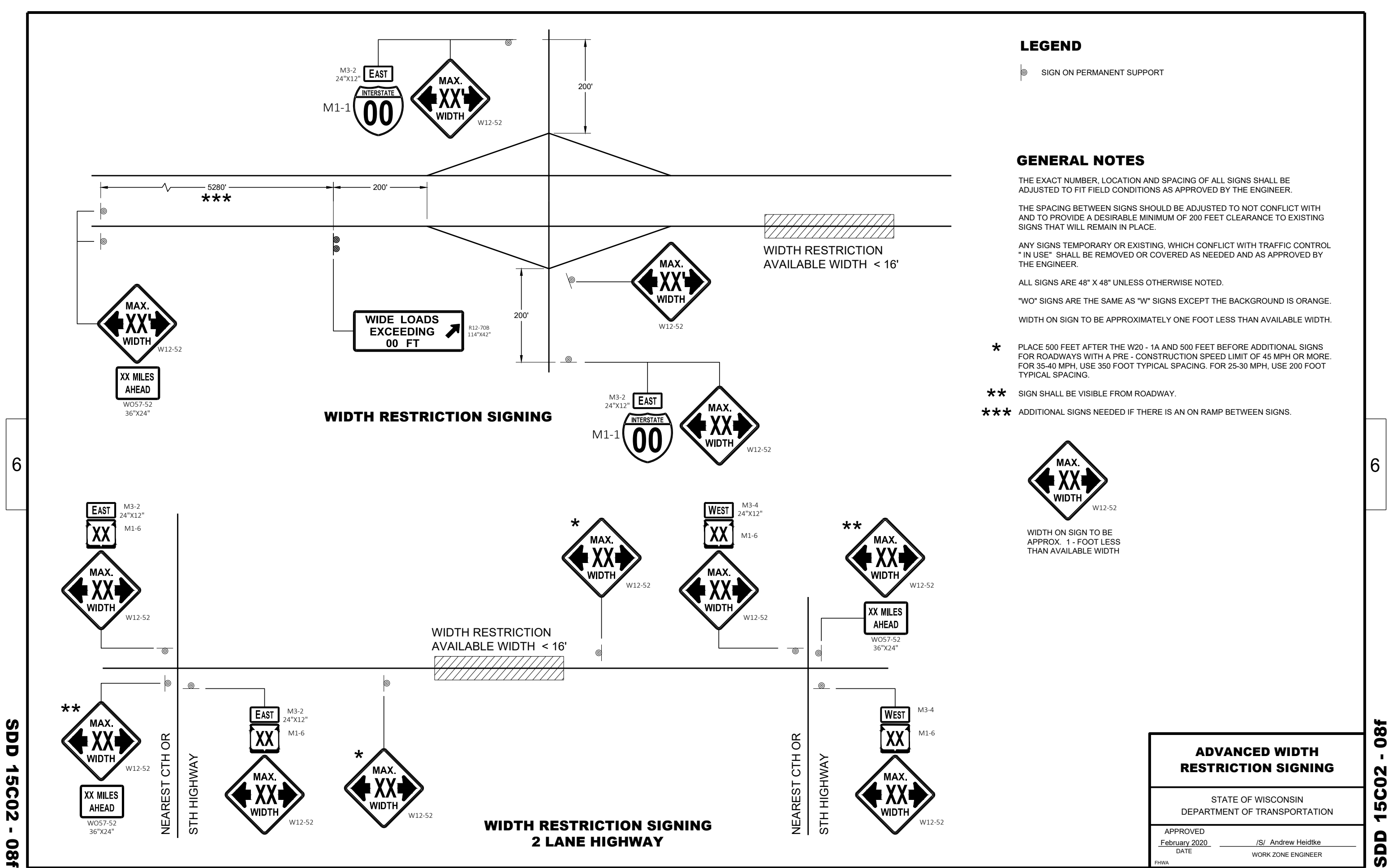
/S/ Jerry H. Zogg

ROADWAY STANDARDS DEVELOPMENT

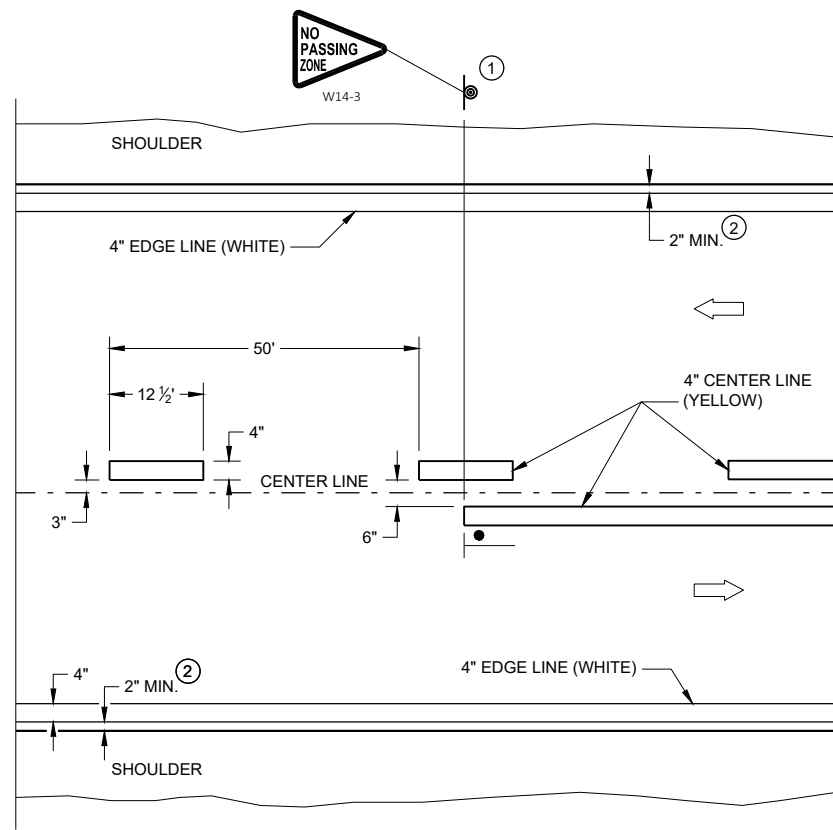
ENGINEER

S.D.D. 14 B 8-2e

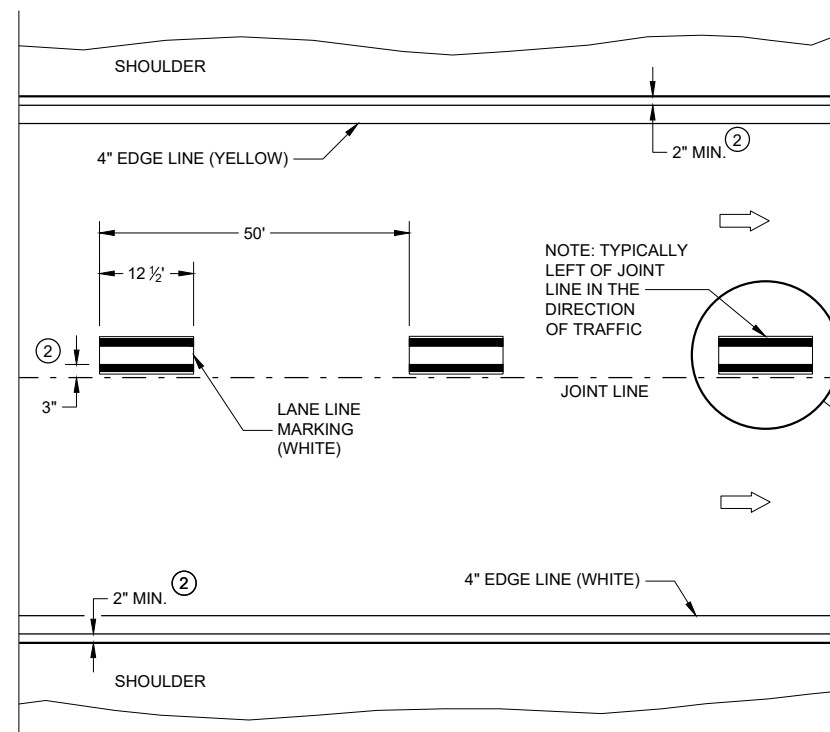
S.D.D. 14 B 8-2e



<b>ADVANCED WIDTH RESTRICTION SIGNING</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

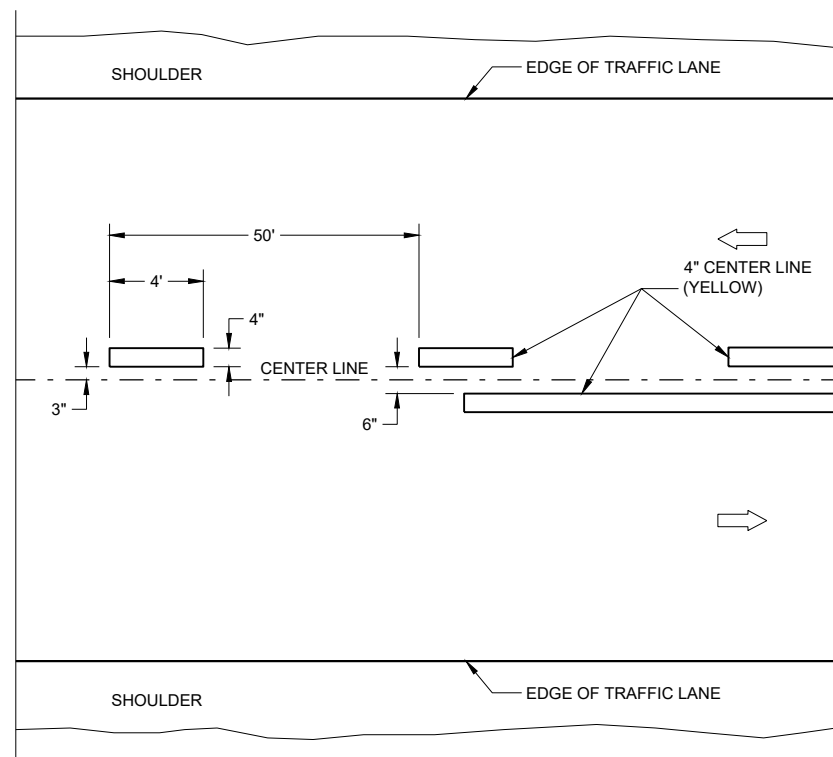


## TWO WAY TRAFFIC

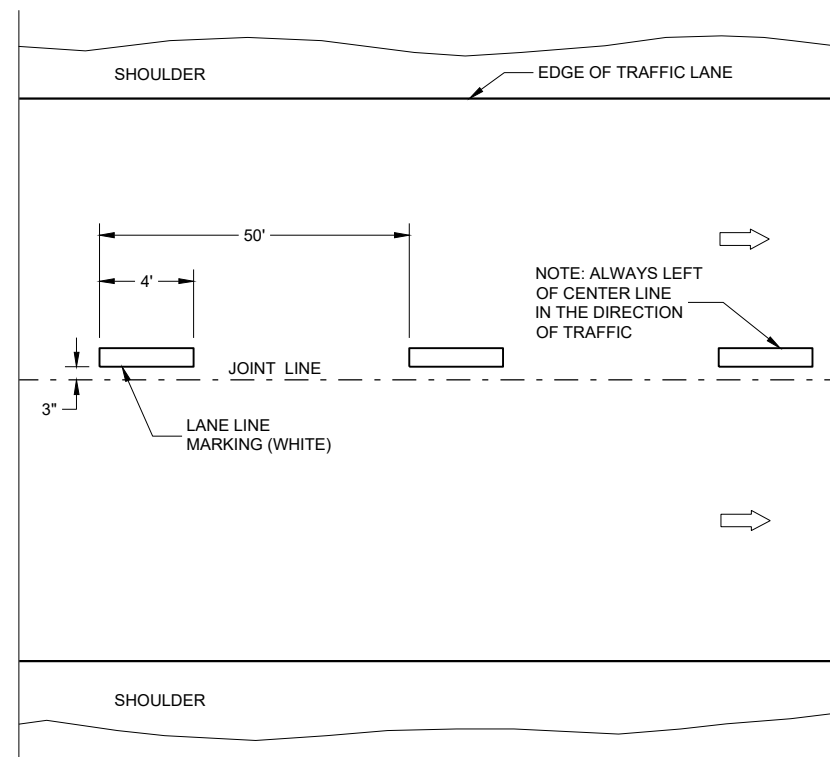


## ONE WAY TRAFFIC

## PERMANENT PAVEMENT MARKING



## TWO WAY TRAFFIC



## ONE WAY TRAFFIC




## TEMPORARY PAVEMENT MARKING

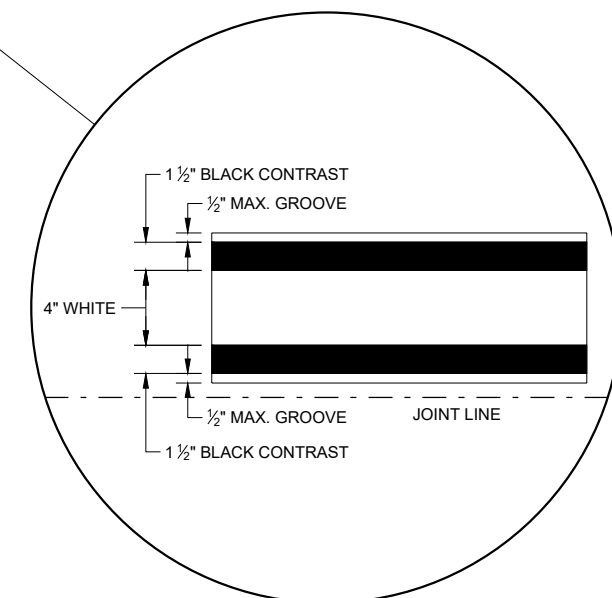
## GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM  
TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITH 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

## LEGEND

-  "T" MARKING  
 SIGN ON PERMANENT SUPPORT  
 DIRECTION OF TRAFFIC



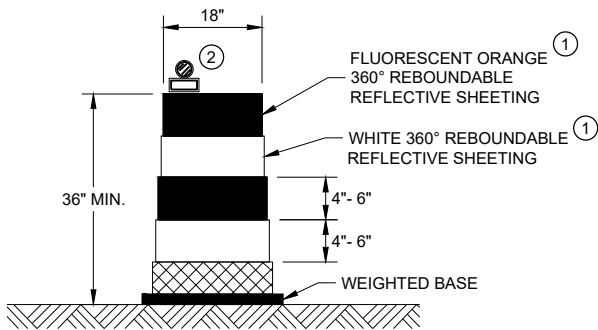
### LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

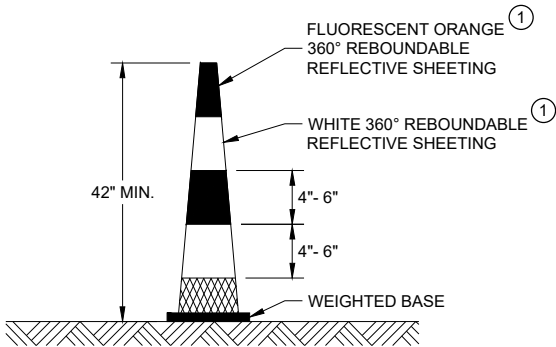
APPROVED  
February 2020  
DATE

/S/ Matthew Rauch  
STATEWIDE SIGNING AND MARKING  
ENGINEER



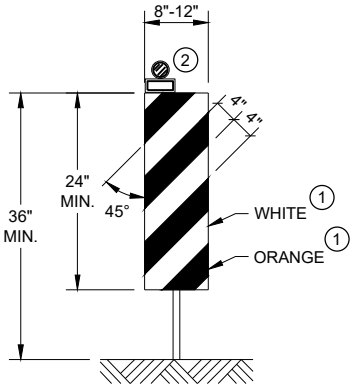


DRUM



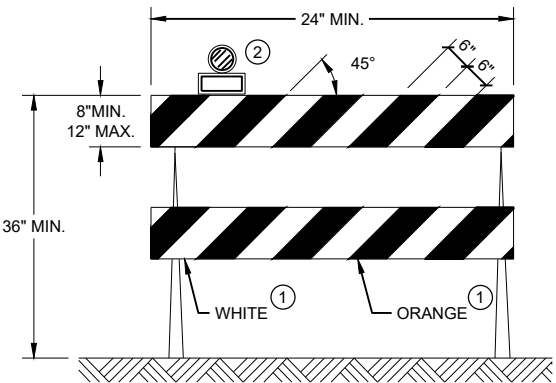
42" CONE

DO NOT USE IN TAPERS  
½ SPACING OF DRUMS



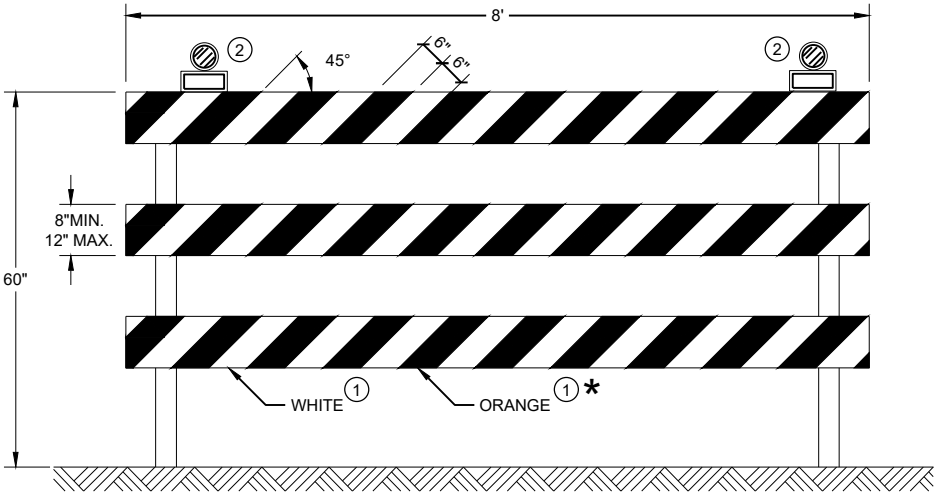
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO  
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES  
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD  
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP  
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES





- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

CHANNELIZING DEVICES  
DRUMS, CONES, BARRICADES  
AND VERTICAL PANELS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

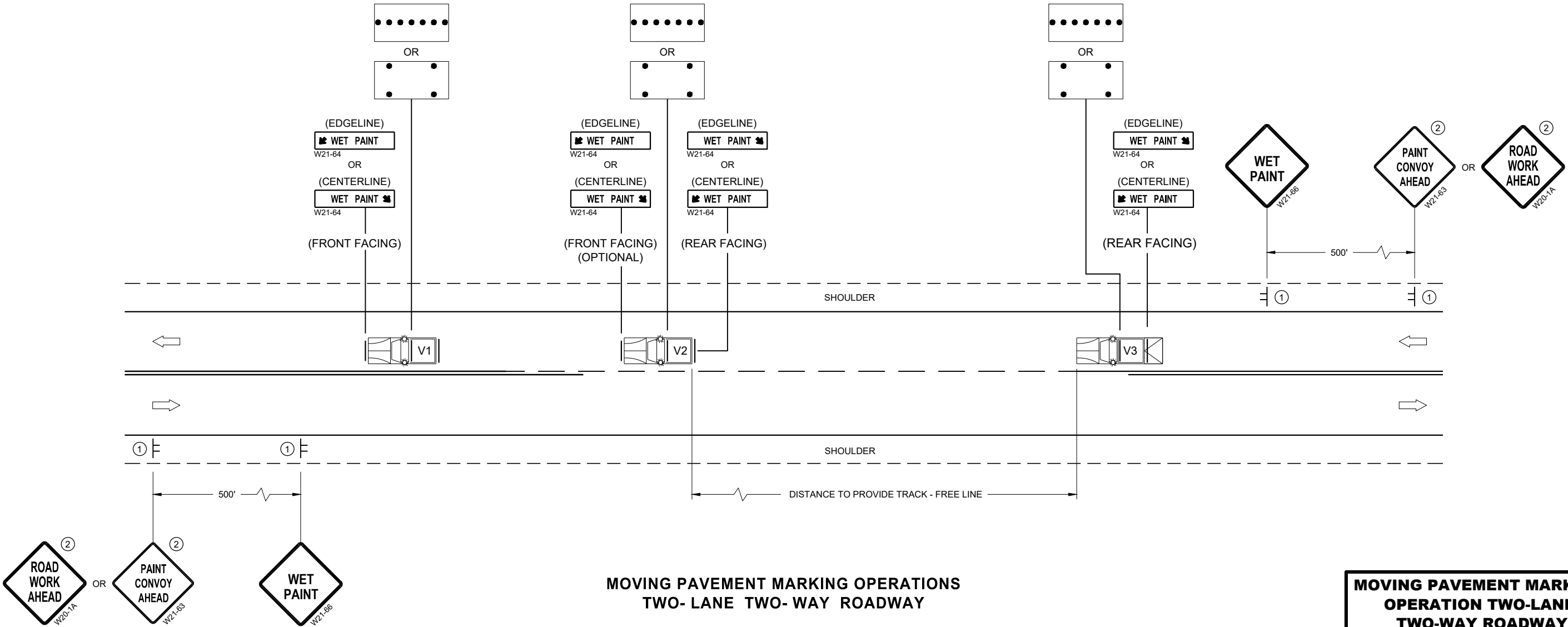
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 18" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

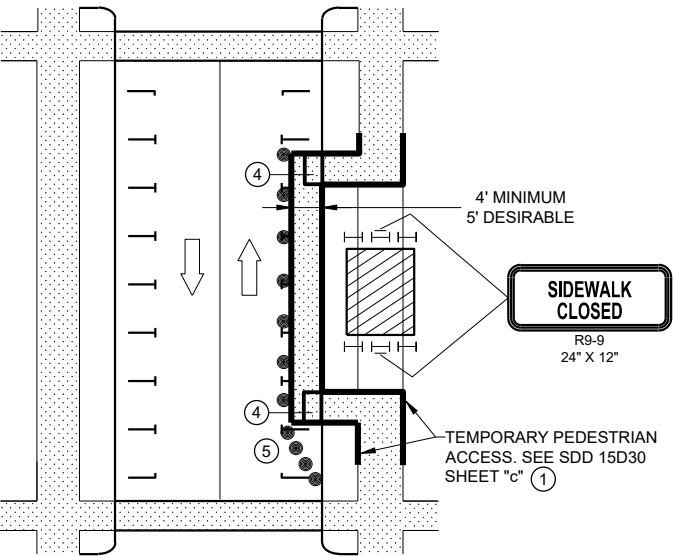


MOVING PAVEMENT MARKING  
OPERATION TWO-LANE  
TWO-WAY ROADWAY

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

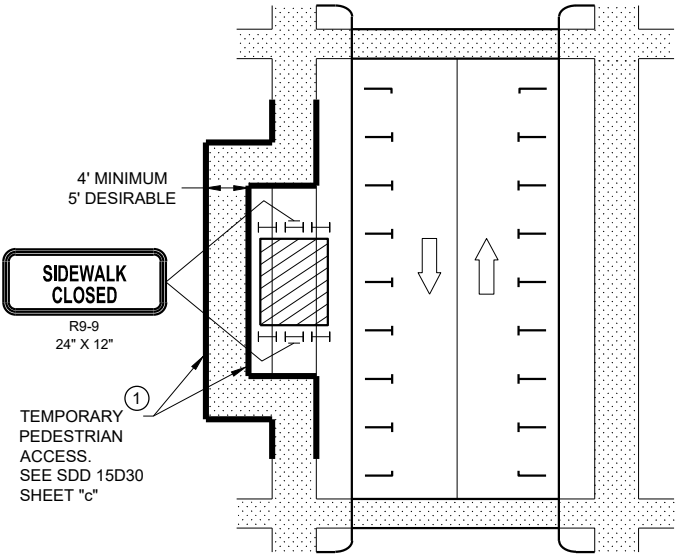
APPROVED  
November 2019 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA

NOTE: MAY BE USED ON ROADWAY WITH POSTED SPEED OF LESS THAN 40 MPH.

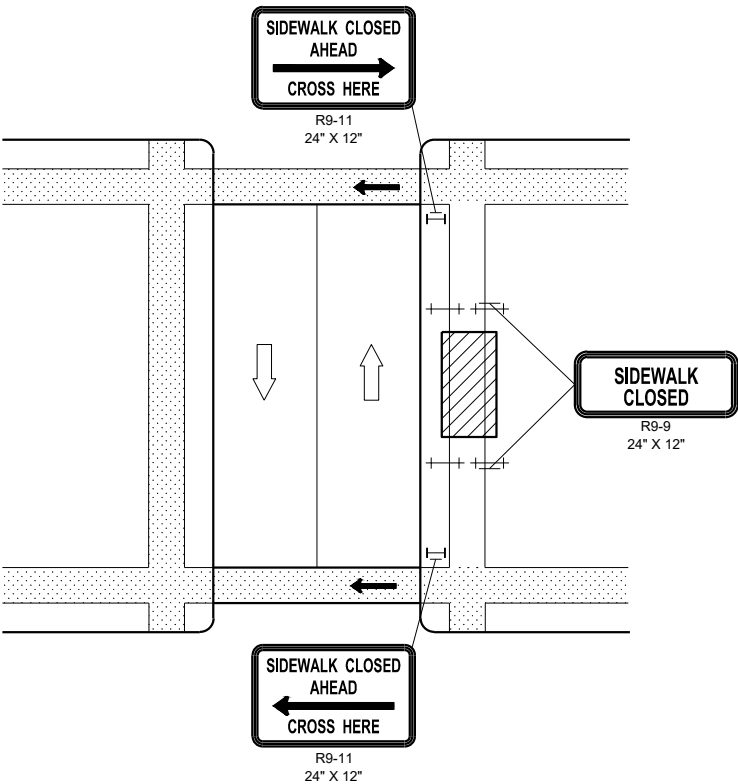


MID-BLOCK SIDEWALK CLOSURE  
IN PARKING LANE

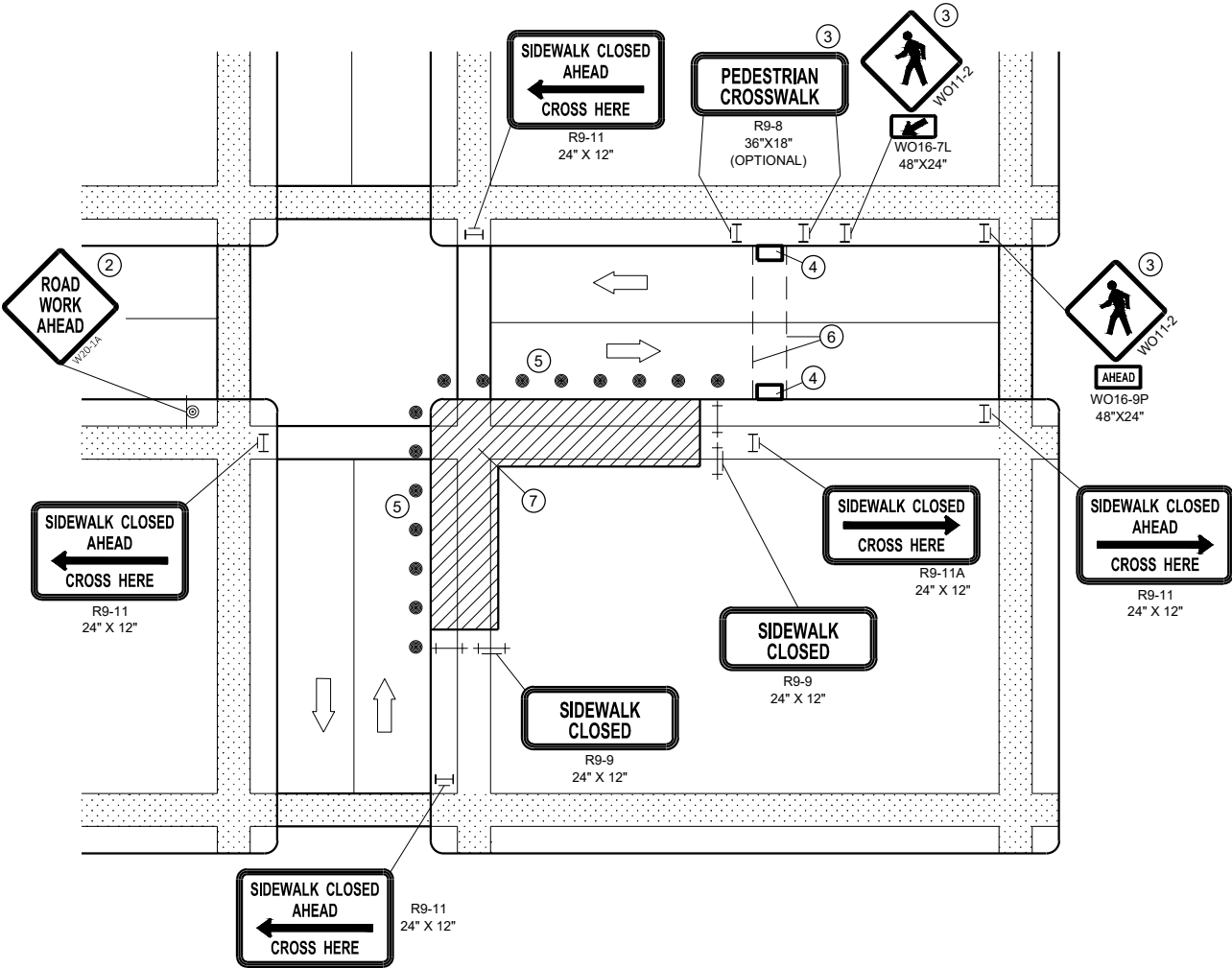
NOTE: LAYOUT SAME AS ABOVE.



SIDEWALK DIVERSION



MID-BLOCK SIDEWALK  
CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN, EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE, USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

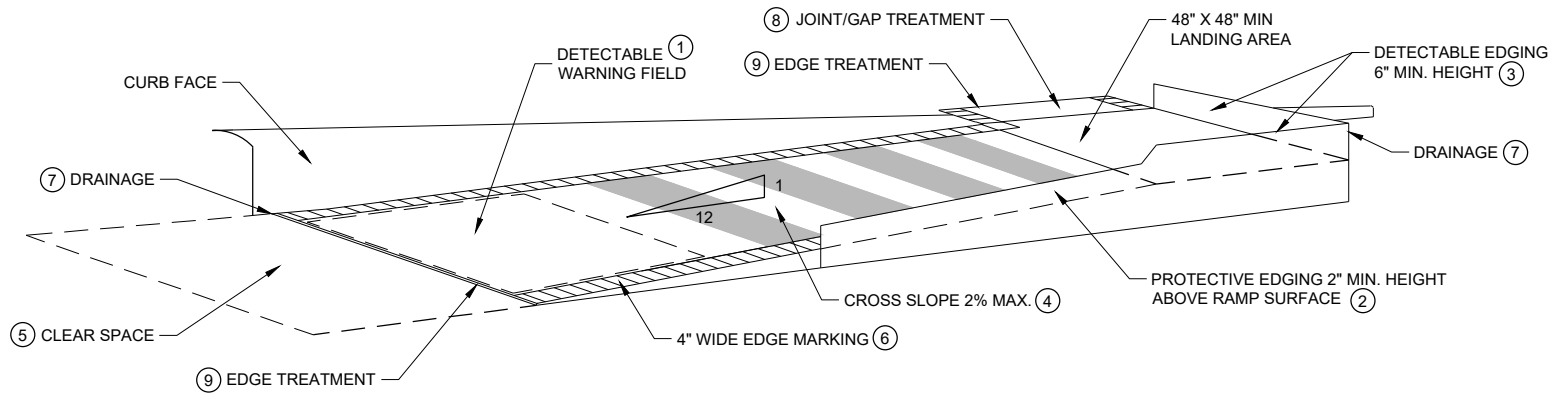
- ① IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE
- ② "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- ③ IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND WO11-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.
- ④ TEMPORARY CURB RAMPS. SEE SDD 15D30 SHEET "b'.
- ⑤ DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- ⑥ TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- ⑦ LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

LEGEND

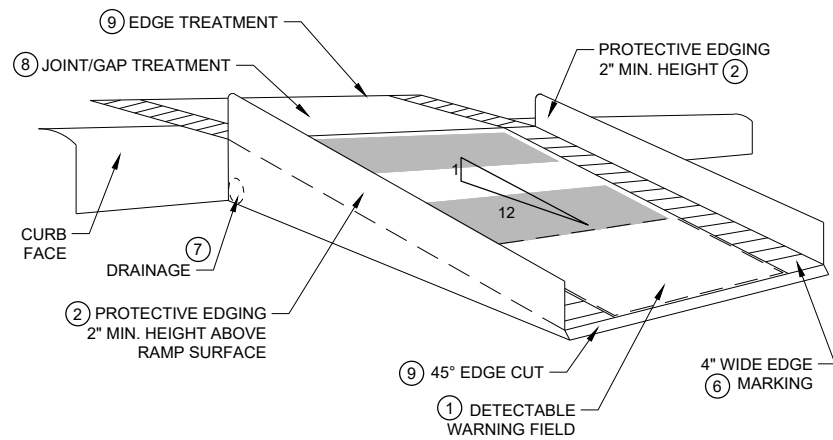
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- PEDESTRIAN CHANNELIZATION DEVICE
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,  
PEDESTRIAN ACCOMMODATION

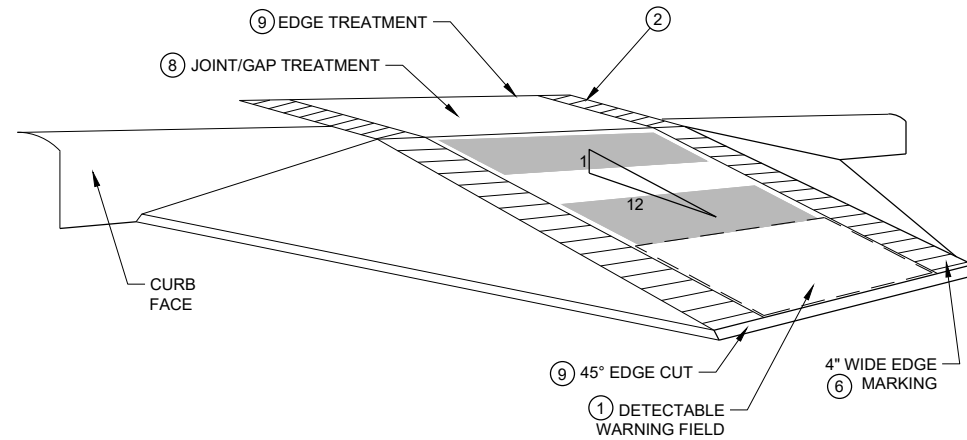
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



TEMPORARY CURB RAMP PARALLEL TO CURB



WITH PROTECTIVE EDGE



WITH SIDE APRON

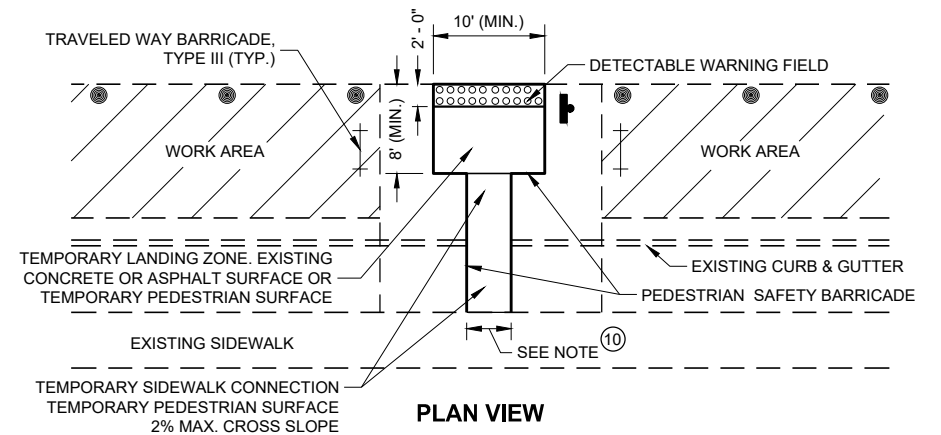
TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

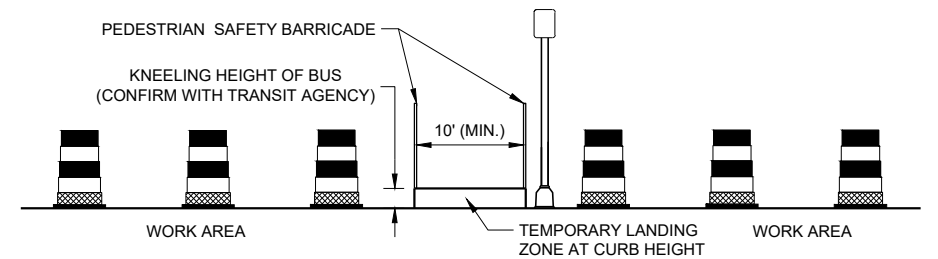
NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- 1 CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 08D05, SHEET "e".
- 2 PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- 3 DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- 4 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
- 5 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
- 6 THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
- 7 DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- 8 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- 9 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- 10 5" WIDE MIN. WITH PEDESTRIAN SAFETY BARRICADE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY BARRICADE.



PLAN VIEW



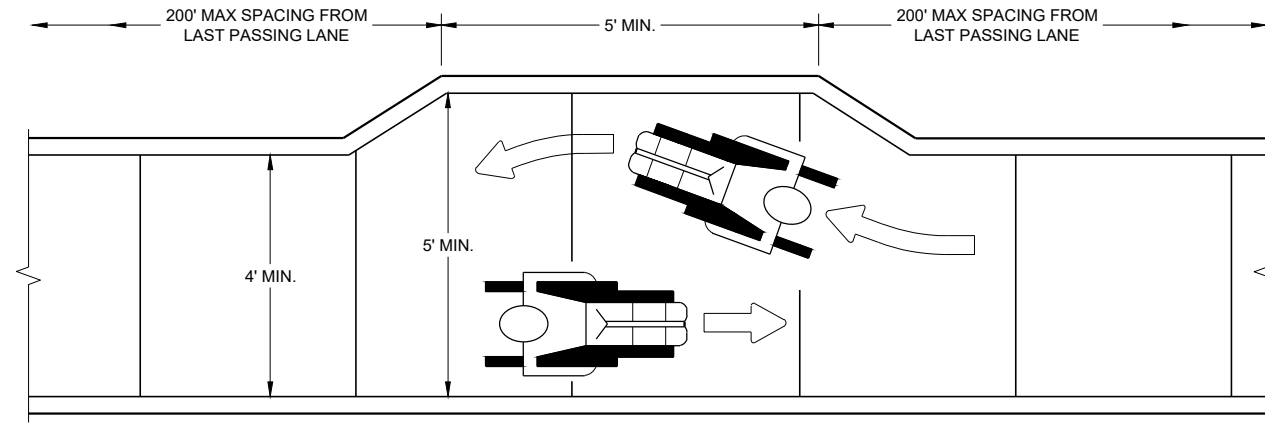
PROFILE VIEW

TEMPORARY BUS STOP PAD

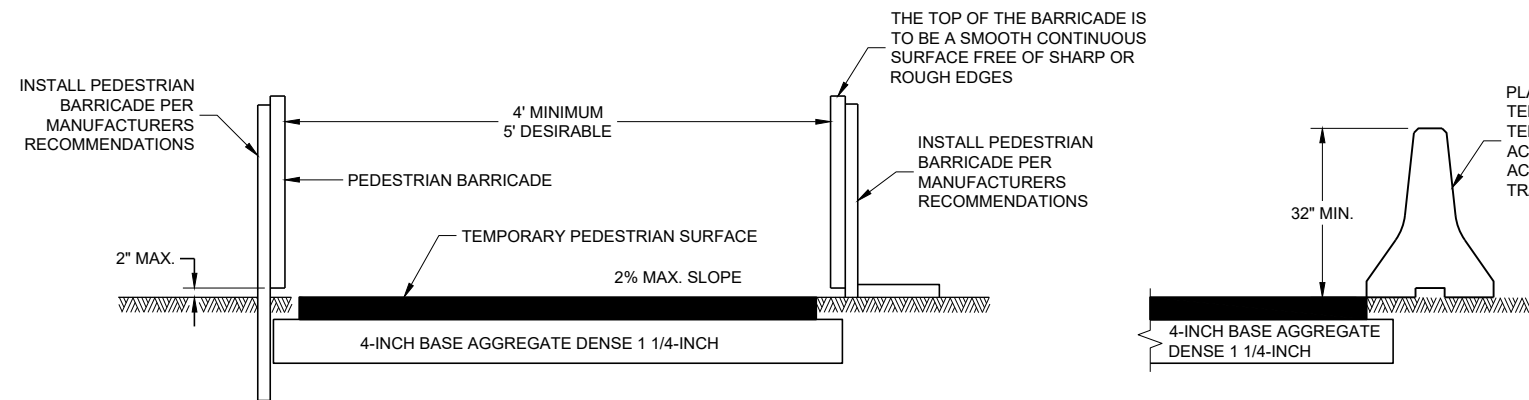
LEGEND

- TRAFFIC CONTROL DRUM
- † TYPE III BARRICADE
- ▨ WORK AREA

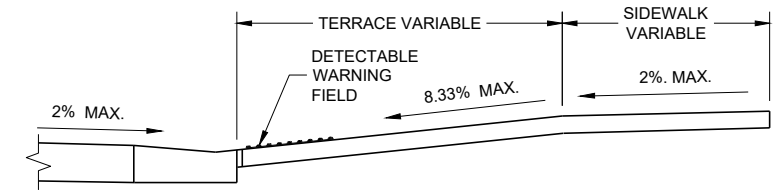
<b>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</b>
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



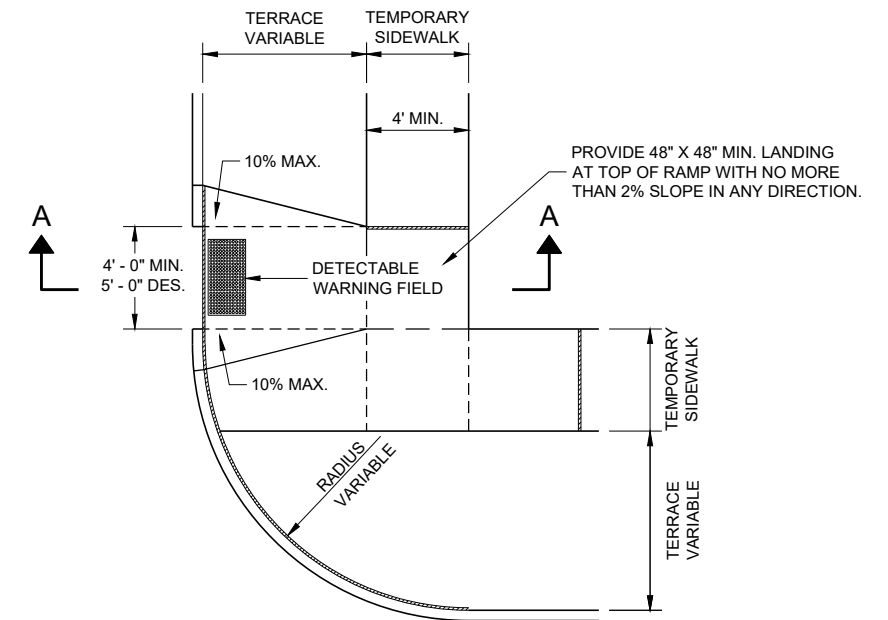
**NARROW SIDEWALK PASSING DETAIL**



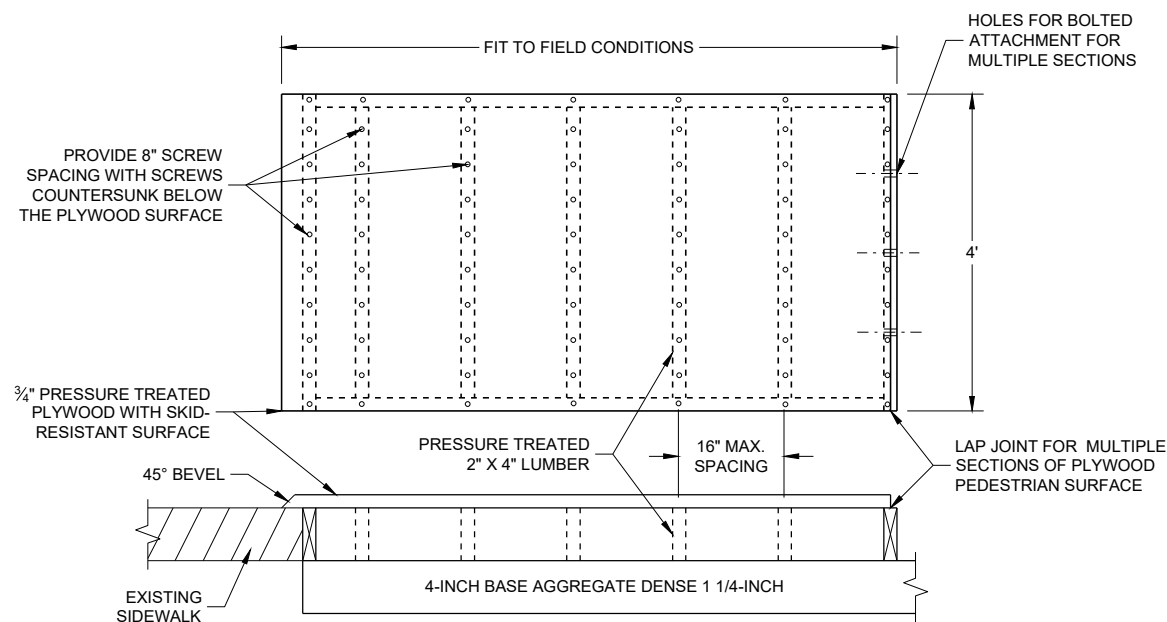
**TEMPORARY PEDESTRIAN ACCESS**



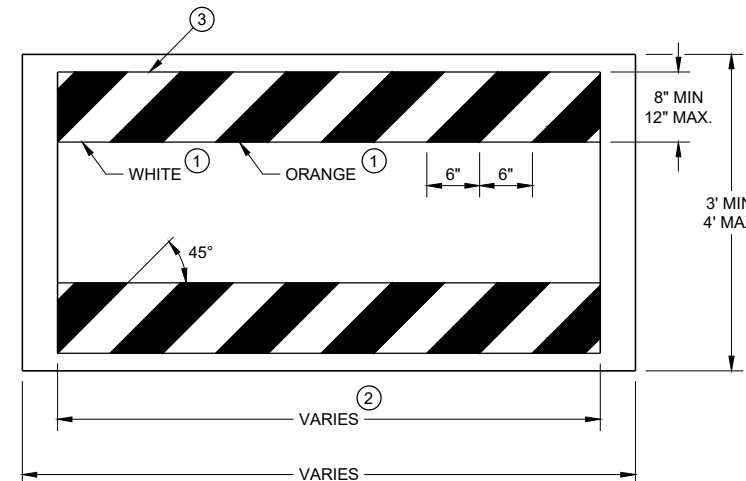
**SECTION A - A**



**PLAN VIEW  
TEMPORARY TYPE 3 RAMP  
(OUTSIDE OF CROSSWALK AREA)**



**TEMPORARY PEDESTRIAN SURFACE PLYWOOD**



**TEMPORARY PEDESTRIAN BARRICADE \***

**GENERAL NOTES**

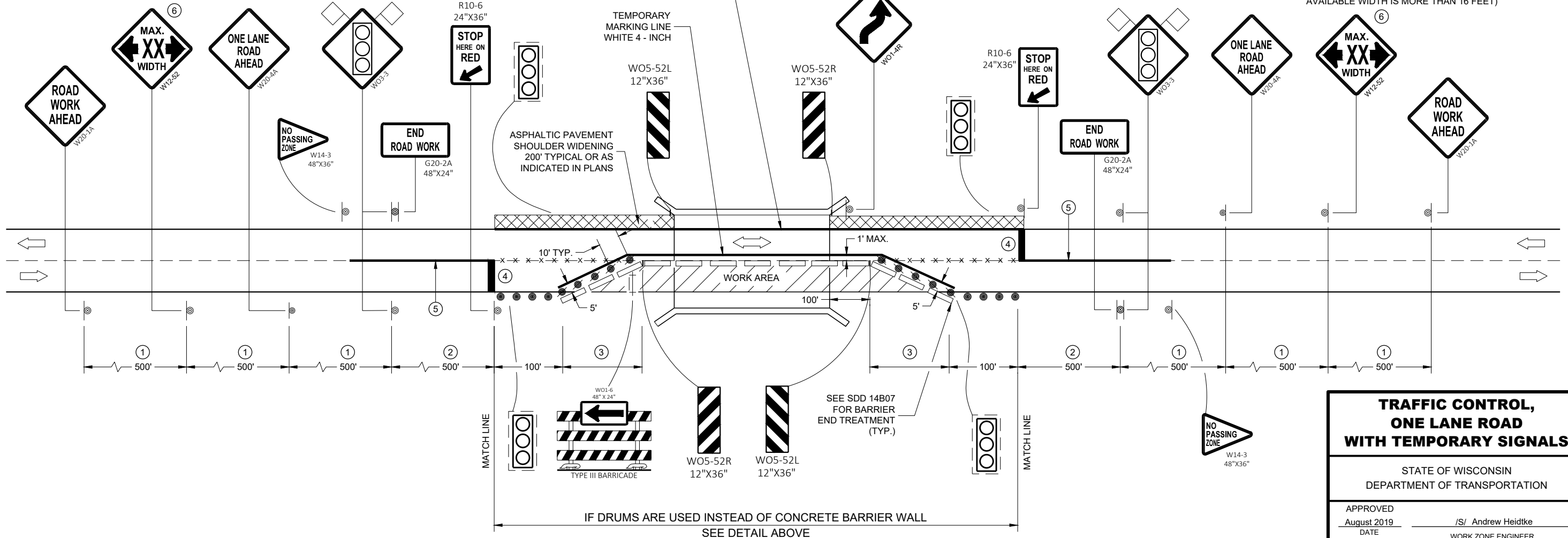
- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- \* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

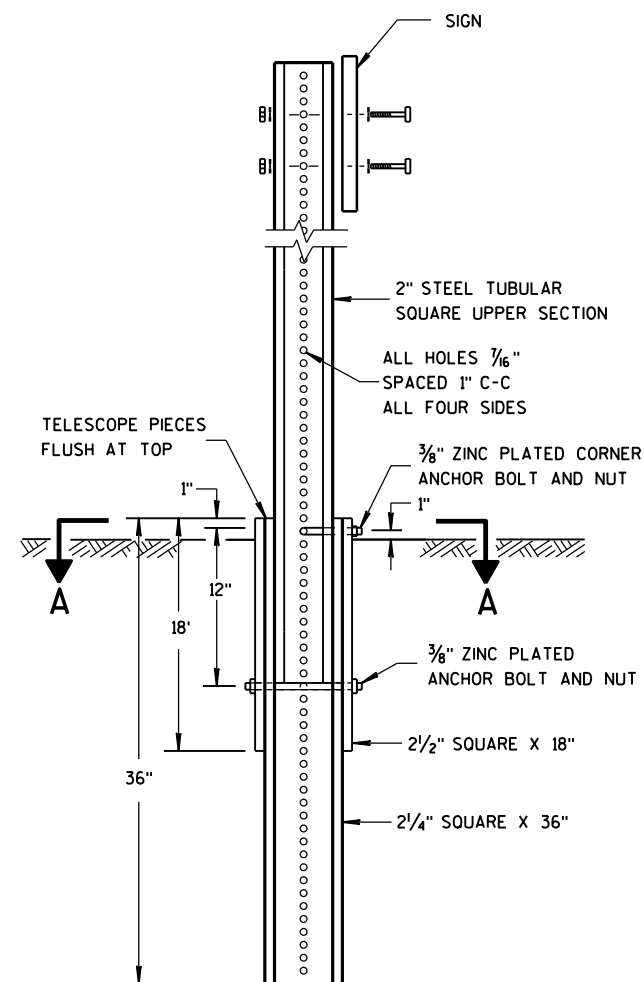
- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLAGS, 16" X 16" MIN. (ORANGE)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- ASPHALTIC PAVEMENT WIDENING
- CONCRETE BARRIER TEMPORARY PRECAST
- TEMPORARY SIGNAL. SEE SDD 09G02 FOR EXACT PLACEMENT

WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)



GENERAL NOTES

- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..
- THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.
- ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.
- "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.
- PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.
- 500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.
  - USE 300 FOOT SPACING IF THE PRE - CONSTRUCTION REGULATORY SPEED IS 35 MPH OR LESS.
  - DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.
  - TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18 - INCH.
  - 700 FOOT TEMPORARY MARKING LINE, DOUBLE YELLOW 4 - INCH . WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
  - SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.

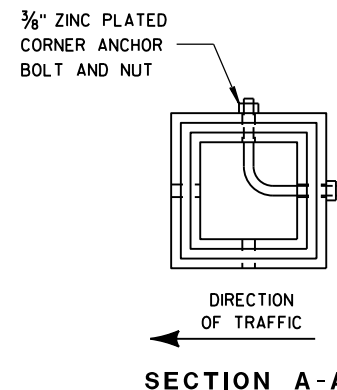


DETAIL OF TUBULAR  
STEEL SIGN POST

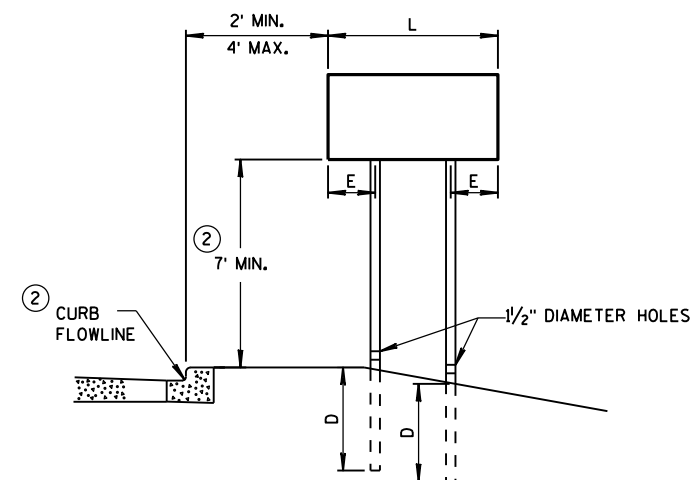
TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL  
BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).  
SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED  
ON TUBULAR STEEL POSTS.



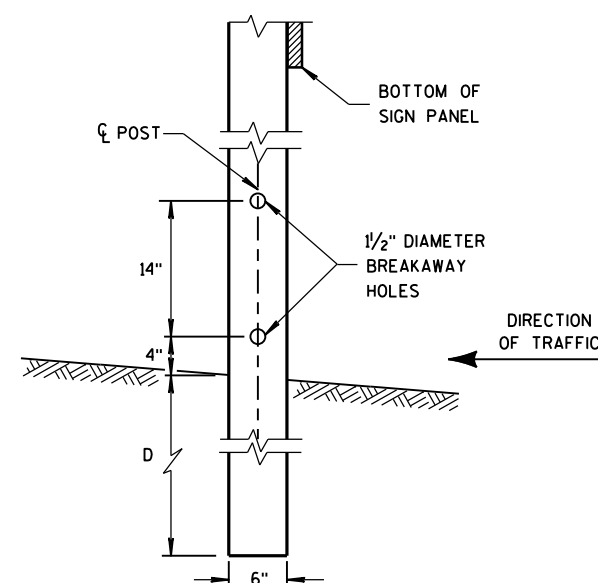
SECTION A-A



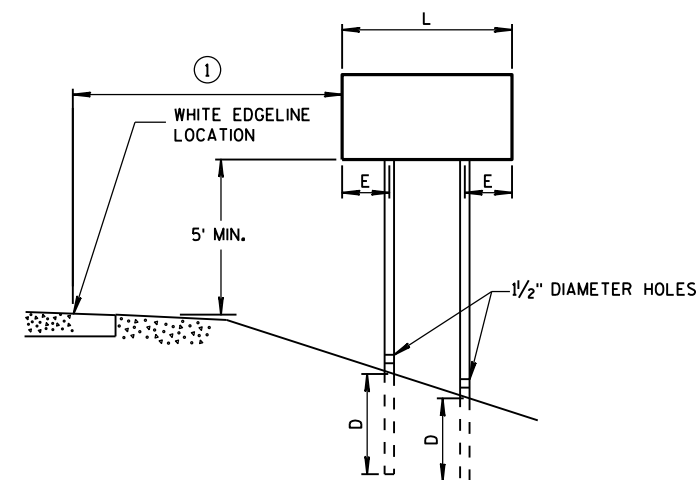
URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH	
AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



4 "x6 " WOOD POST  
MODIFICATION



RURAL AREA

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

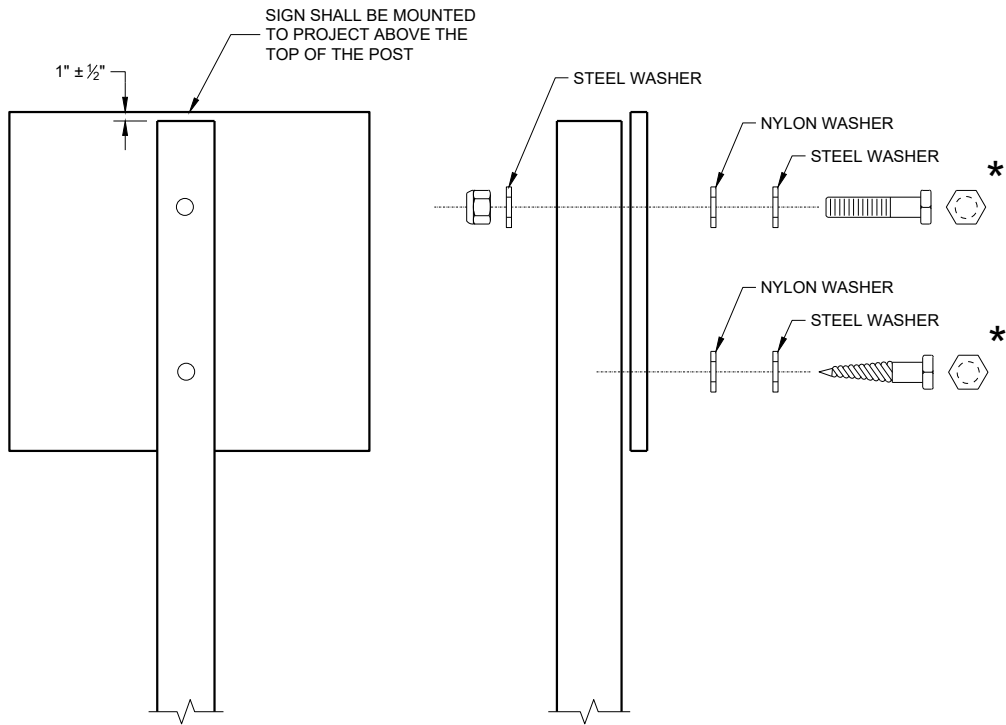
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL  
SIGN MOUNTING

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS  
SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM  
DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM  
DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH  
SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED  
COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POST (4" x 6")  
LAG SCREWS - 3/8" x 3"  
MACHINE BOLTS - 5/16" x 6 1/2" OR 7" LENGTH W/NUTS

SQUARE STEEL POST (2" x 2")  
MACHINE BOLTS - 3/8" x 3 1/4" LENGTH W/NUTS  
RIVETS - 3/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM  
BODY/MANDREL O.D. FLANGE 0.720 - 0.765 INCH,  
GRIP RANGE 0.042 - 0.375 INCH

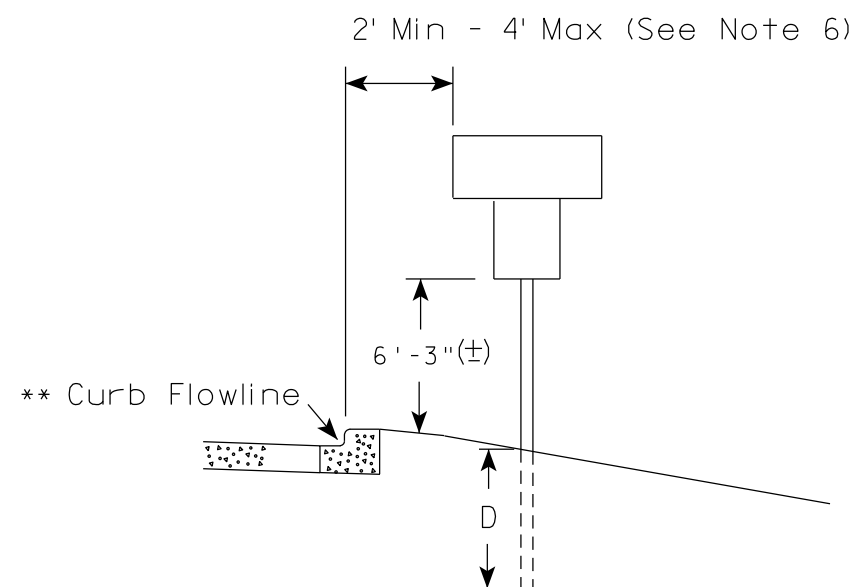
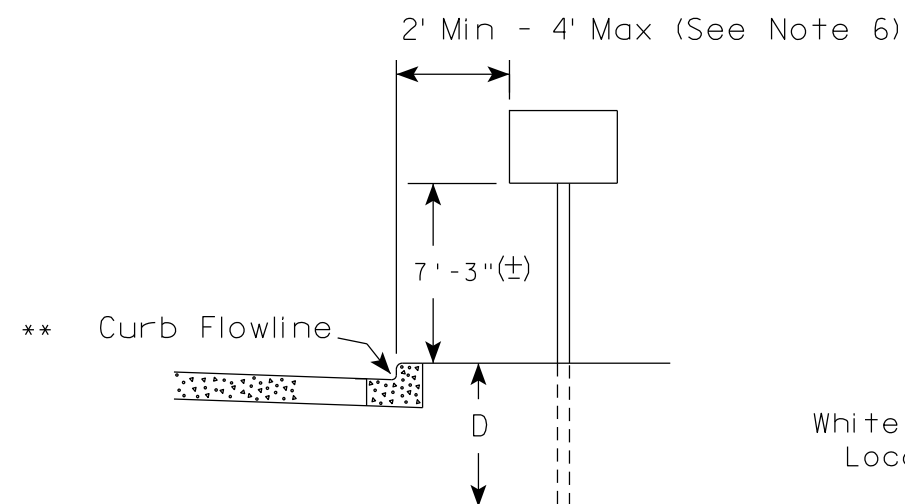
WASHERS (ALL POSTS) -  
1 1/4" O.D. x 3/8" I.D. x 1/16" STEEL  
1 1/4" O.D. x 3/8" I.D. x 0.080 NYLON

\* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION  
PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM  
SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH  
THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER  
THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

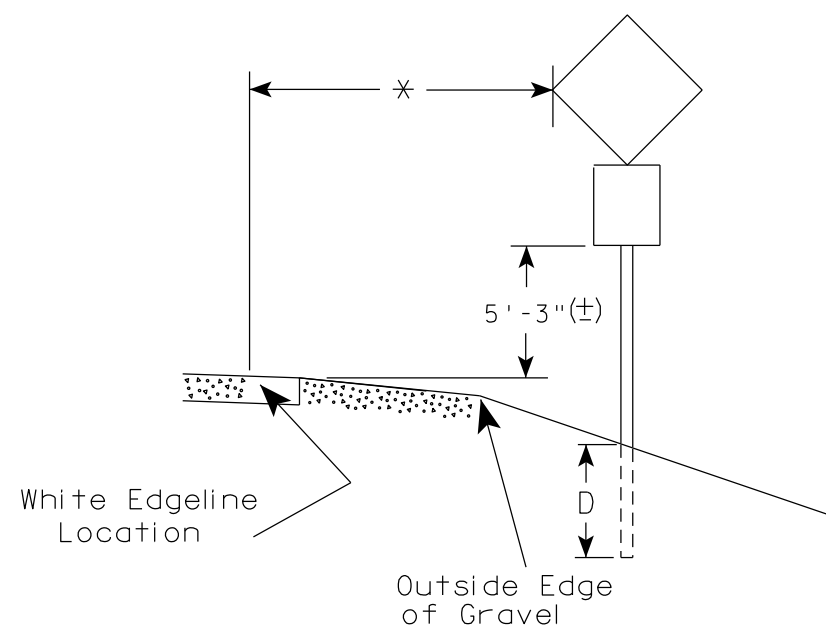
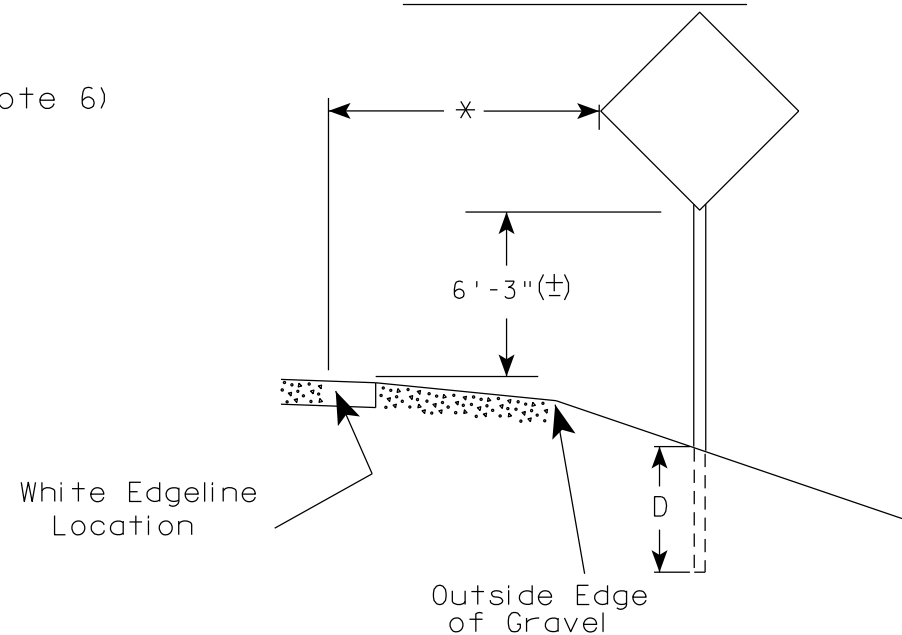


# URBAN AREA



\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

# RURAL AREA (See Note 2)



\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

## POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

## GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.  
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

TYPICAL INSTALLATION  
OF PERMANENT TYPE II  
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22

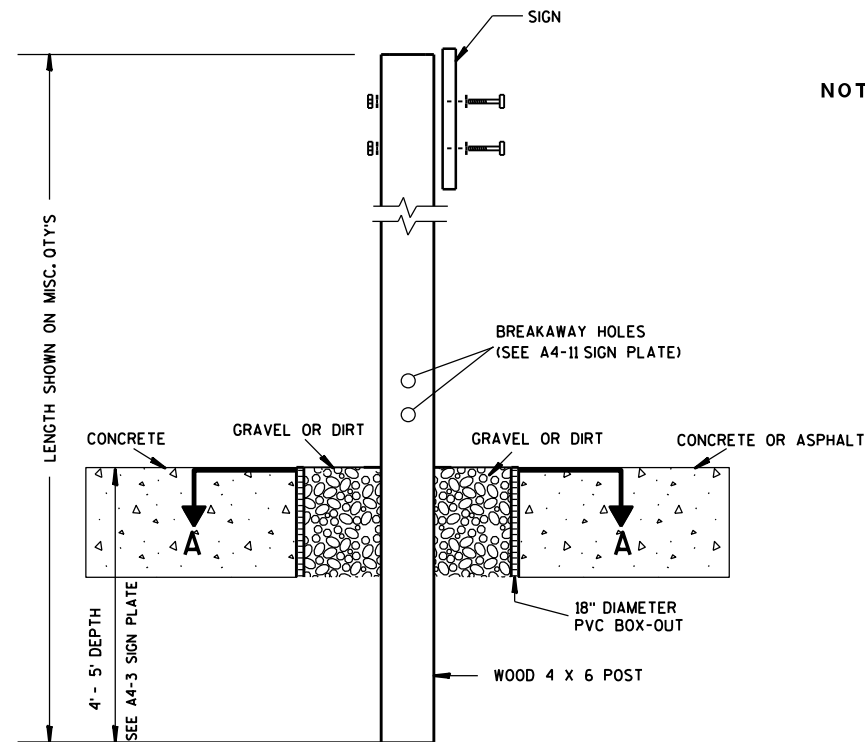
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

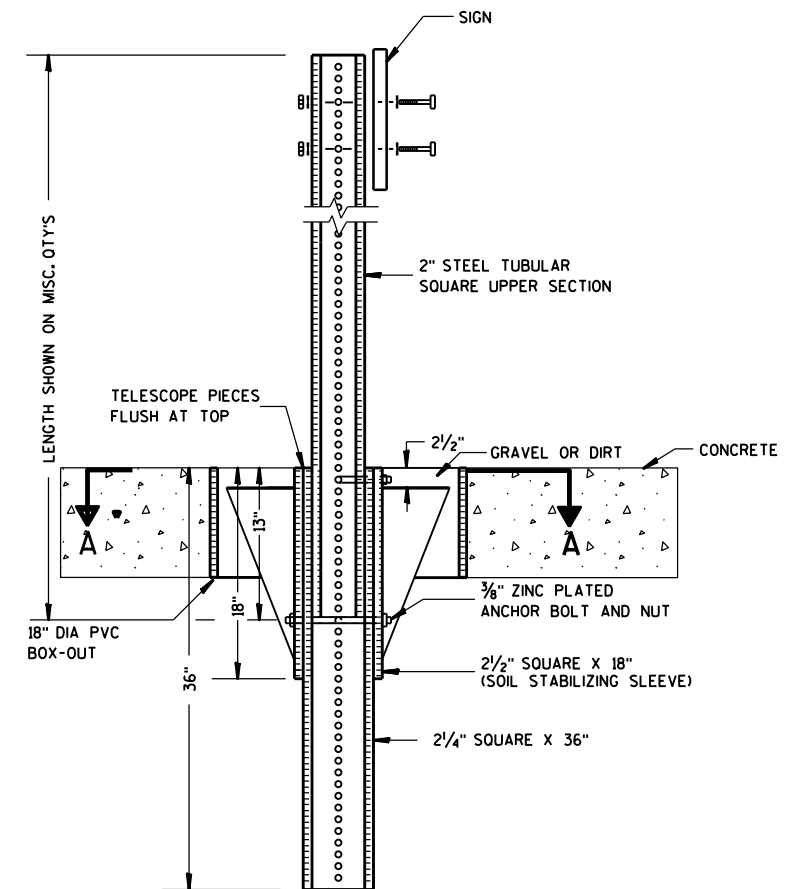
E



### ELEVATION VIEW

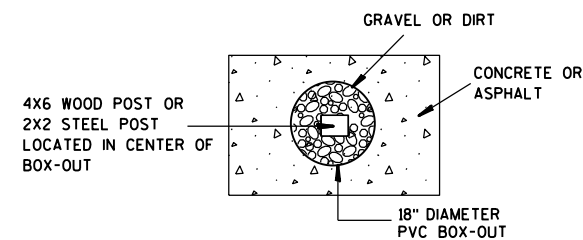
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



### ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



### PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST  
BOX-OUTS  
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

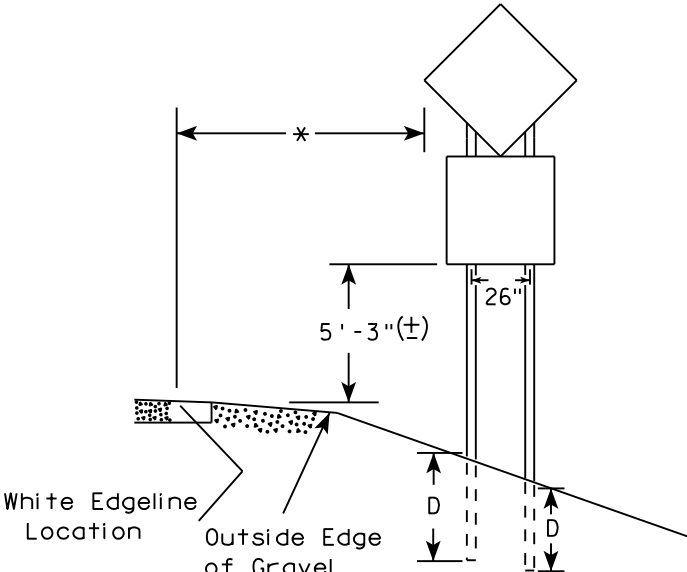
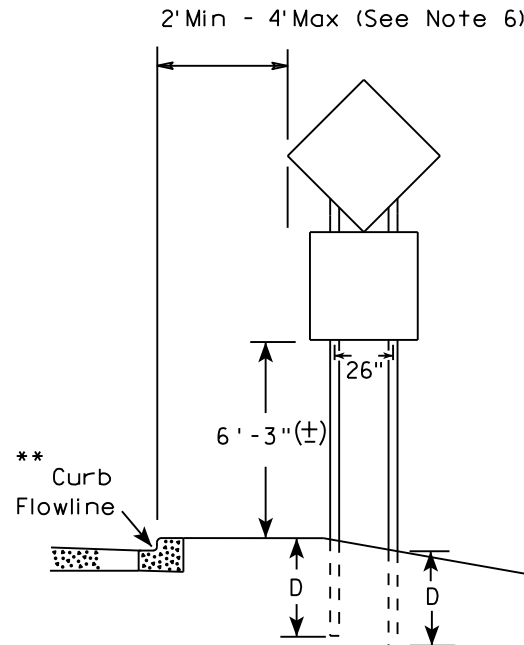
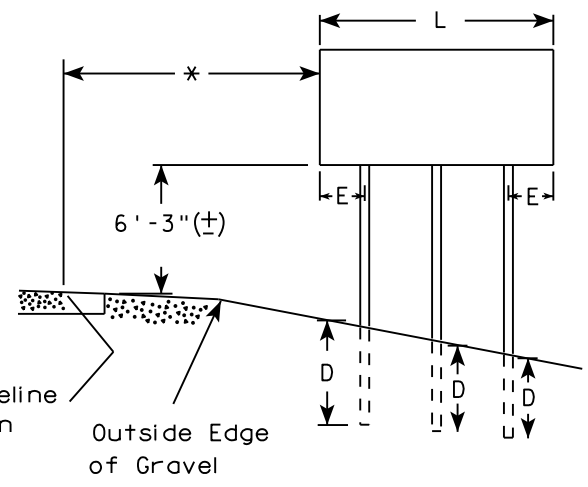
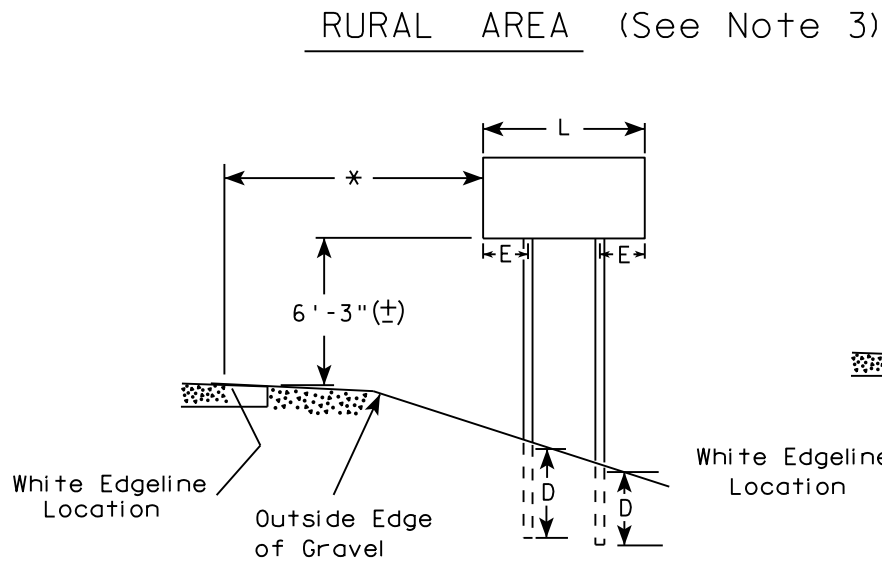
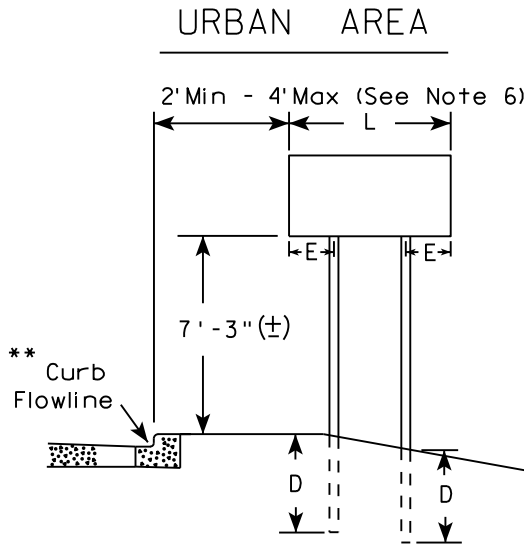
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



\*\*\*

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

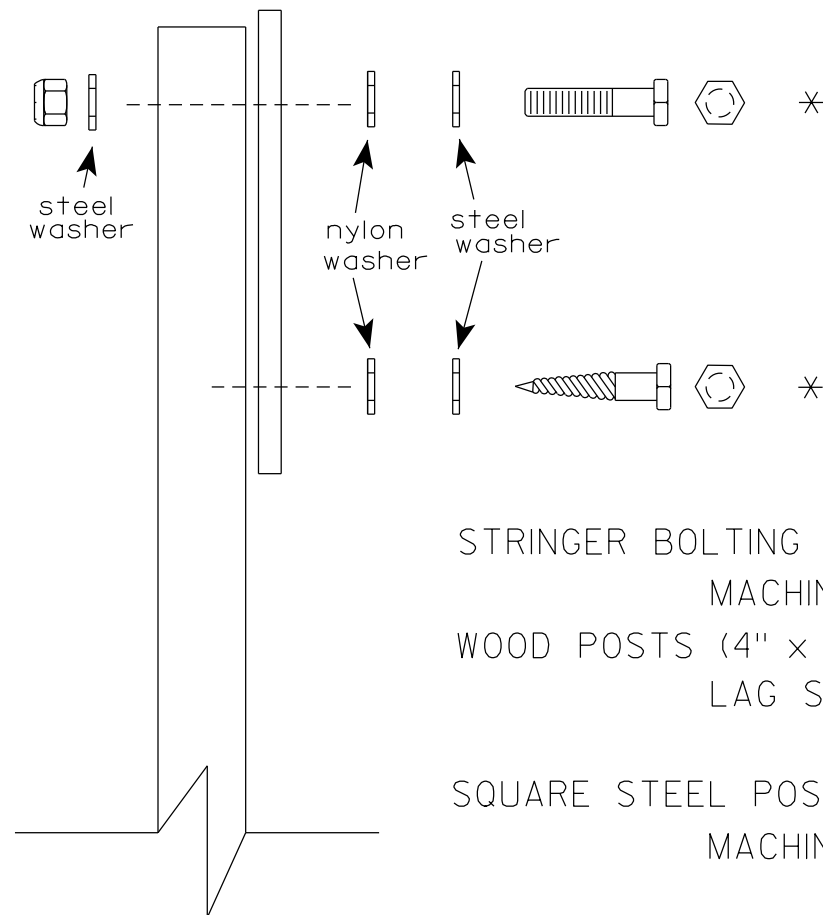
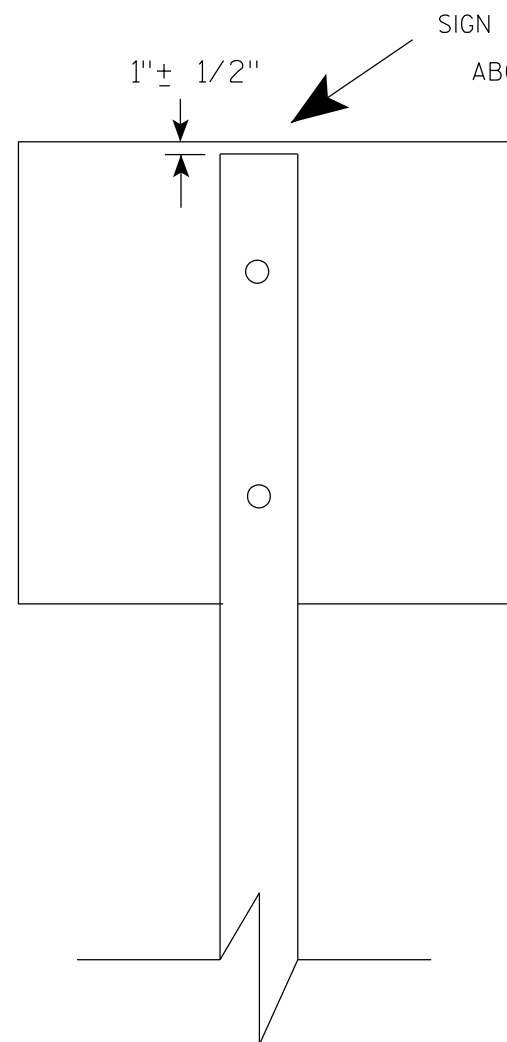
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH	
Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 8/21/17	PLATE NO. A4-4.15

- GENERAL NOTES
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
  2. See tables below for required number of posts.
  3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
  4. The (±) tolerance for mounting height is 3 inches.
  5. J-Assemblies are considered to be one sign for mounting height.
  6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
  7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
  8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

- \* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- \*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- \*\*\* See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

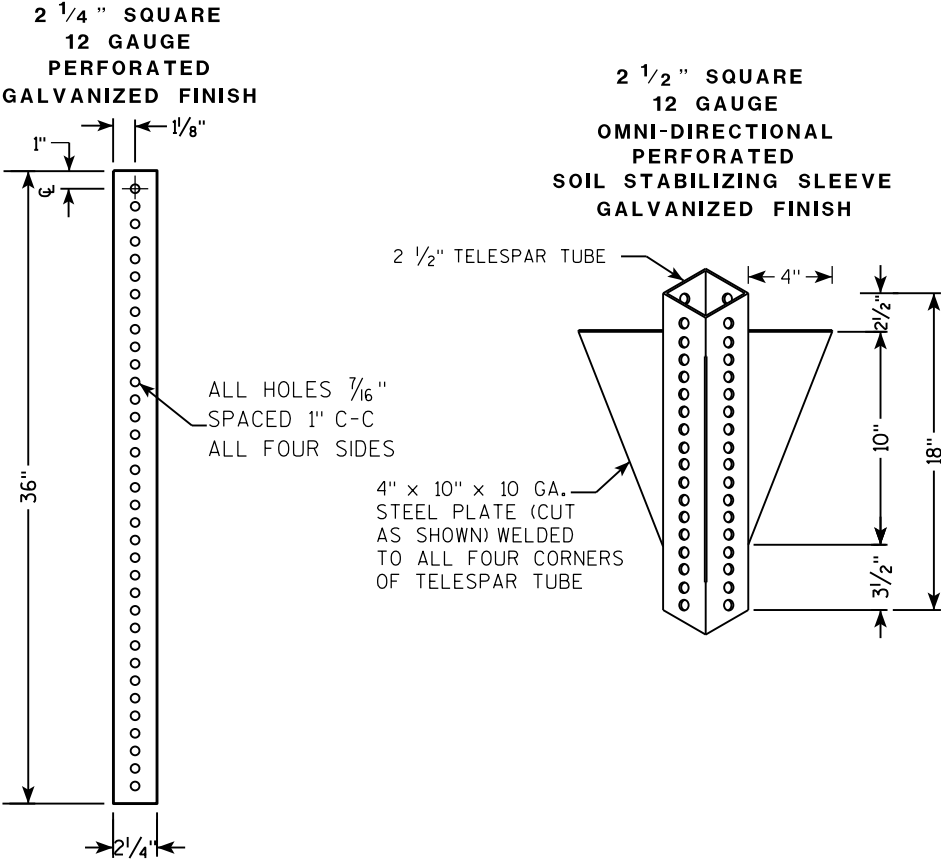
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS -  $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS -  $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)  
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS -  $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)  
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS -  $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL  
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ " STEEL
  - 1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X .080 NYLON

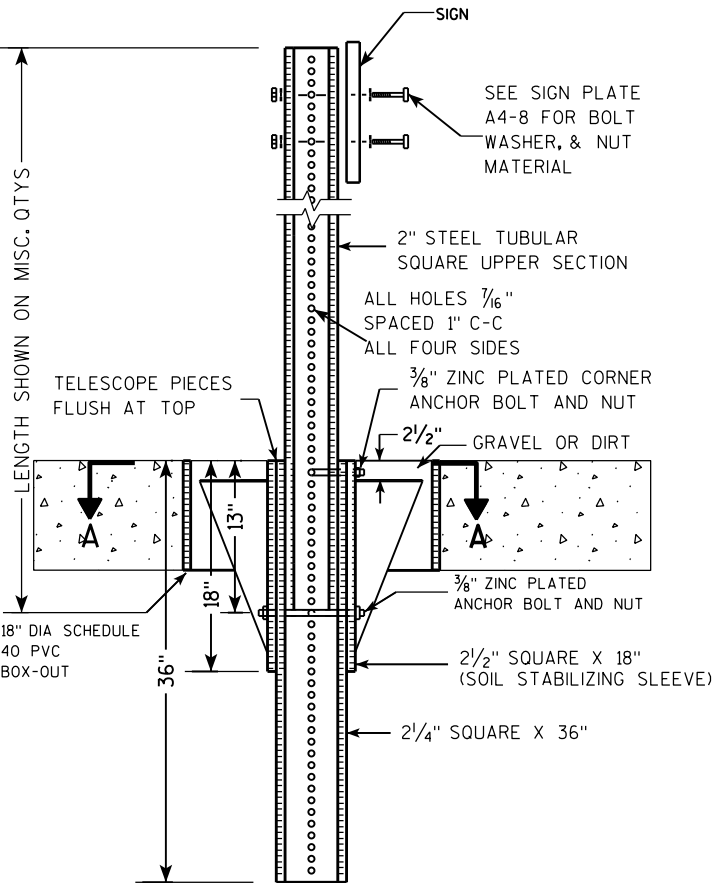
\* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

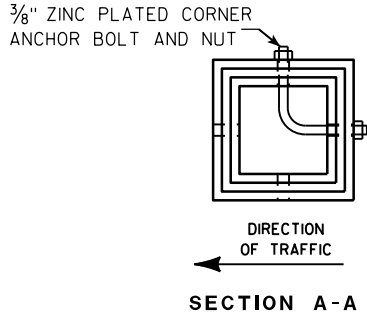
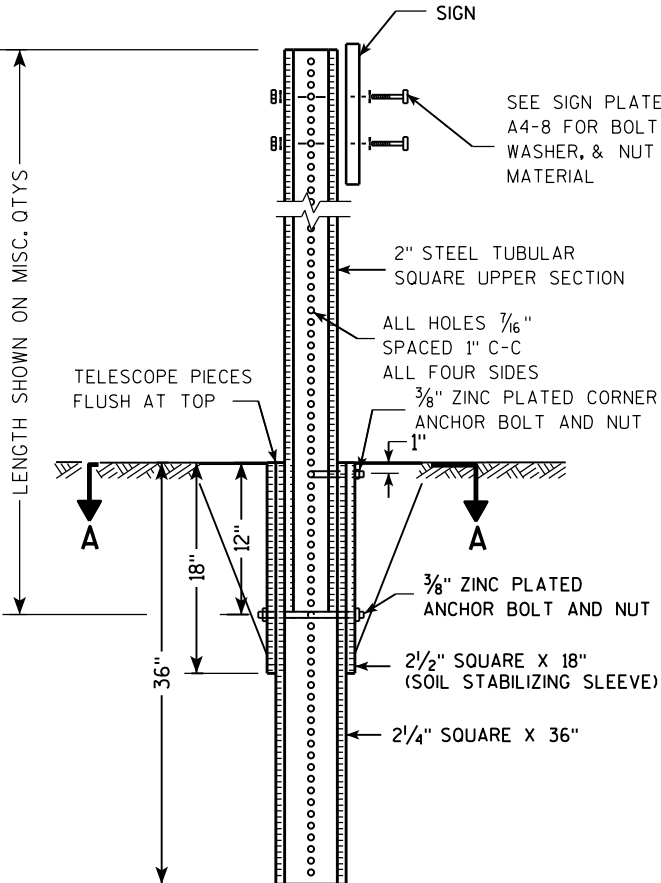
TELESCOPIC TUBING ANCHORS  
TWO PIECE SYSTEM



DETAIL OF TUBULAR STEEL SIGN POST  
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST  
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

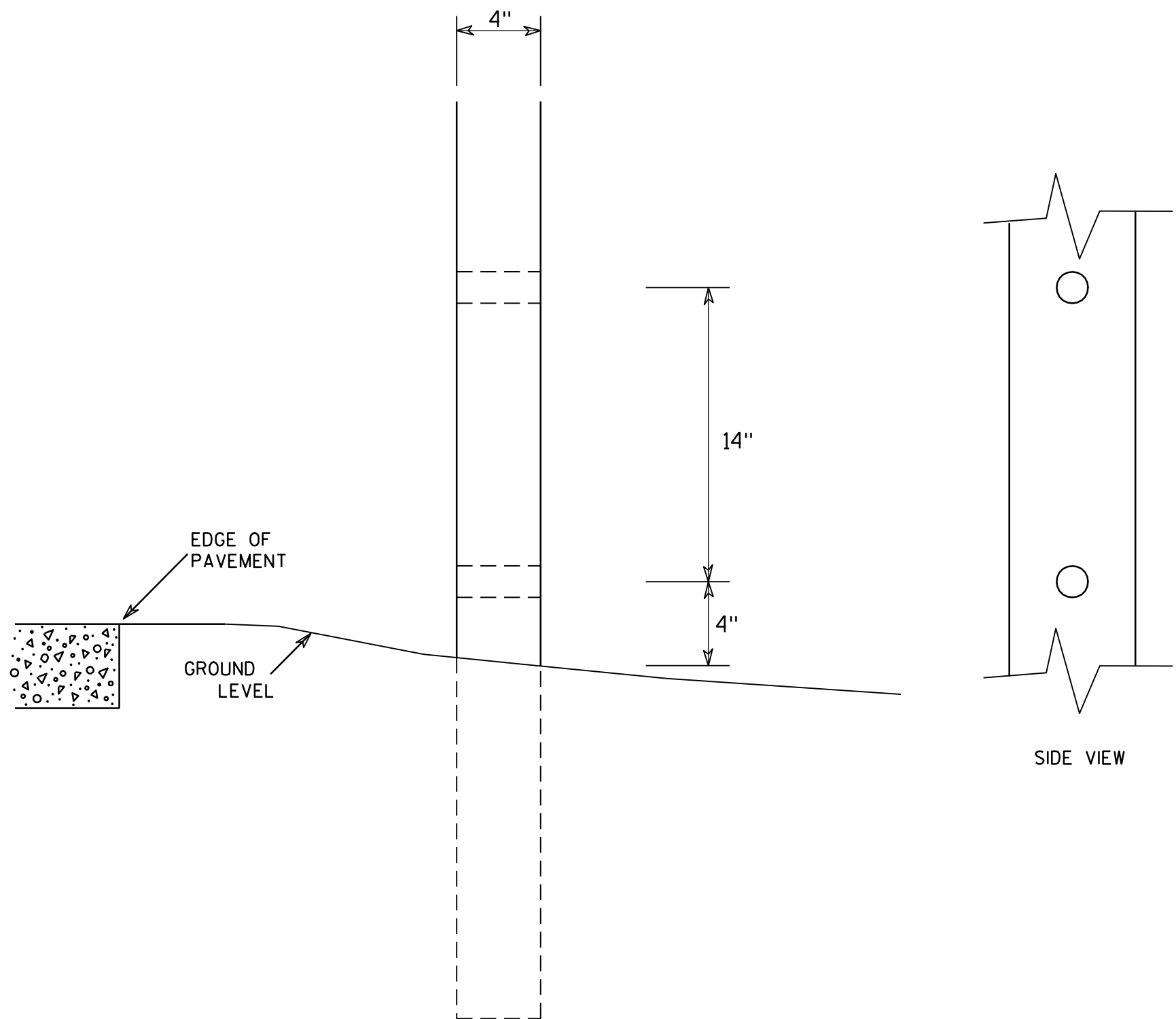
TUBULAR STEEL  
SIGN POST  
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

7



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

4 X 6 WOOD POST  
MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Chester J. Spang*  
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

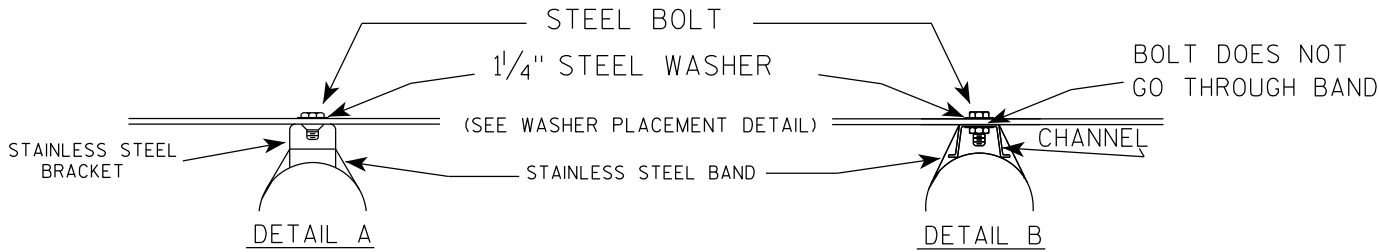
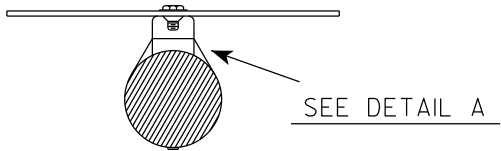
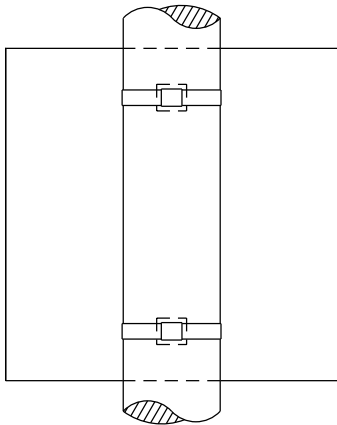
COUNTY:

SHEET NO:

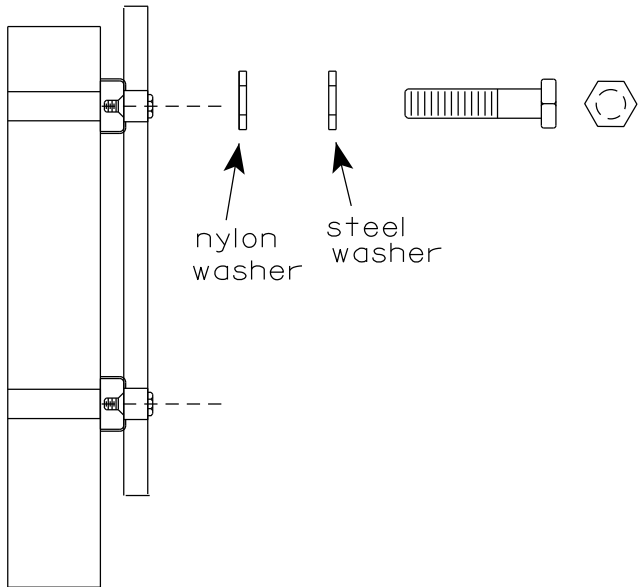
E

BANDING

SINGLE SIGN



WASHER PLACEMENT

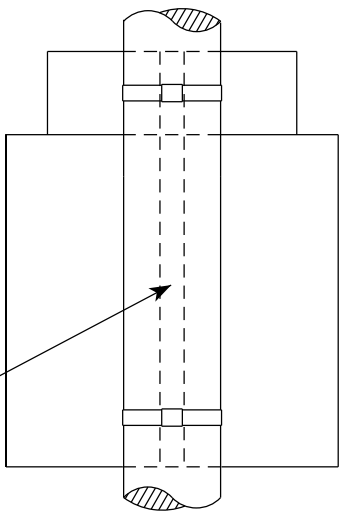


WASHERS (ALL POSTS) -  
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL  
1-1/4" O.D. X 3/8" I.D. X .080 NYLON  
FOR ALL TYPE H SIGNS

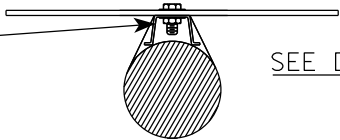
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



CHANNEL  
SEE TYPICAL PANEL  
INSTALLATION SHEET

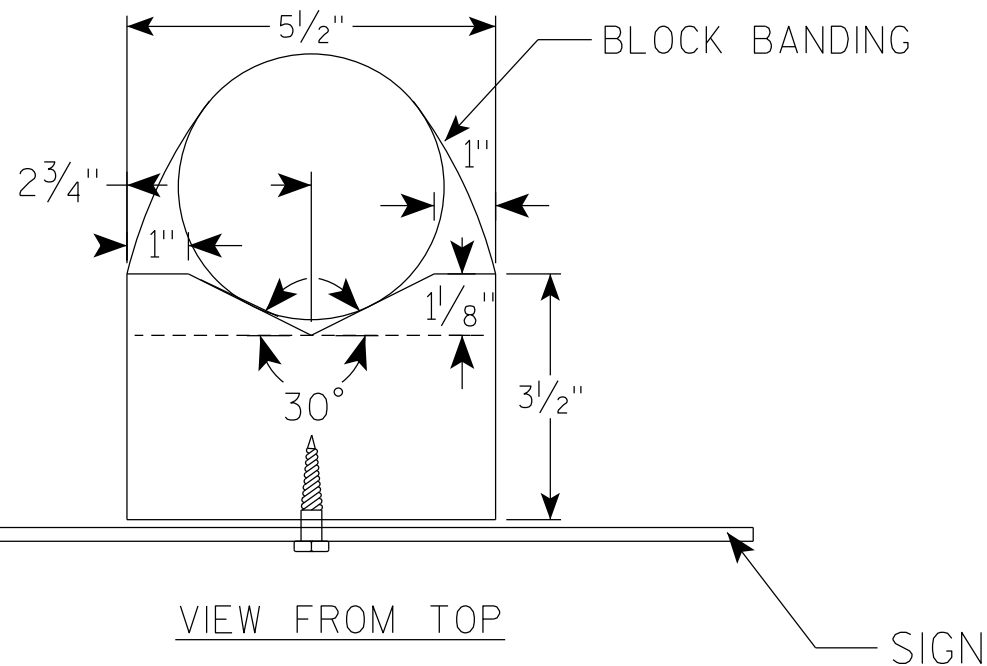
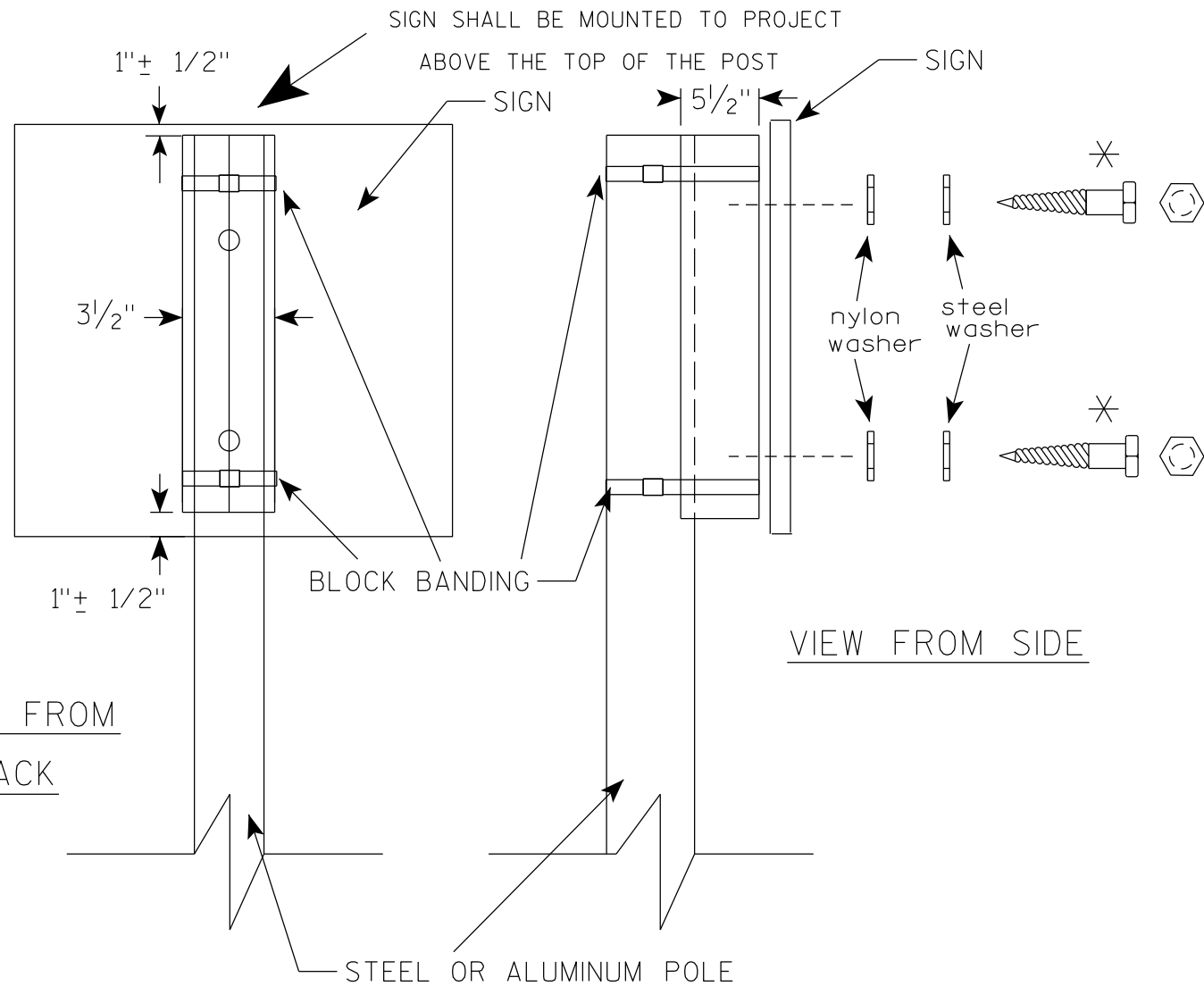


STANDARD SIGN  
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer  
DATE 6/10/19 PLATE NO. A5-9.4

VIEW FROM  
BACK



## GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL,  $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE  $1\frac{1}{4}$ " O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE  $1\frac{1}{4}$ " O.D. X  $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE  $\frac{3}{8}$ " X  $2\frac{1}{2}$ "

BLOCK BANDING DETAIL  
( V-BLOCK OPTION )

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

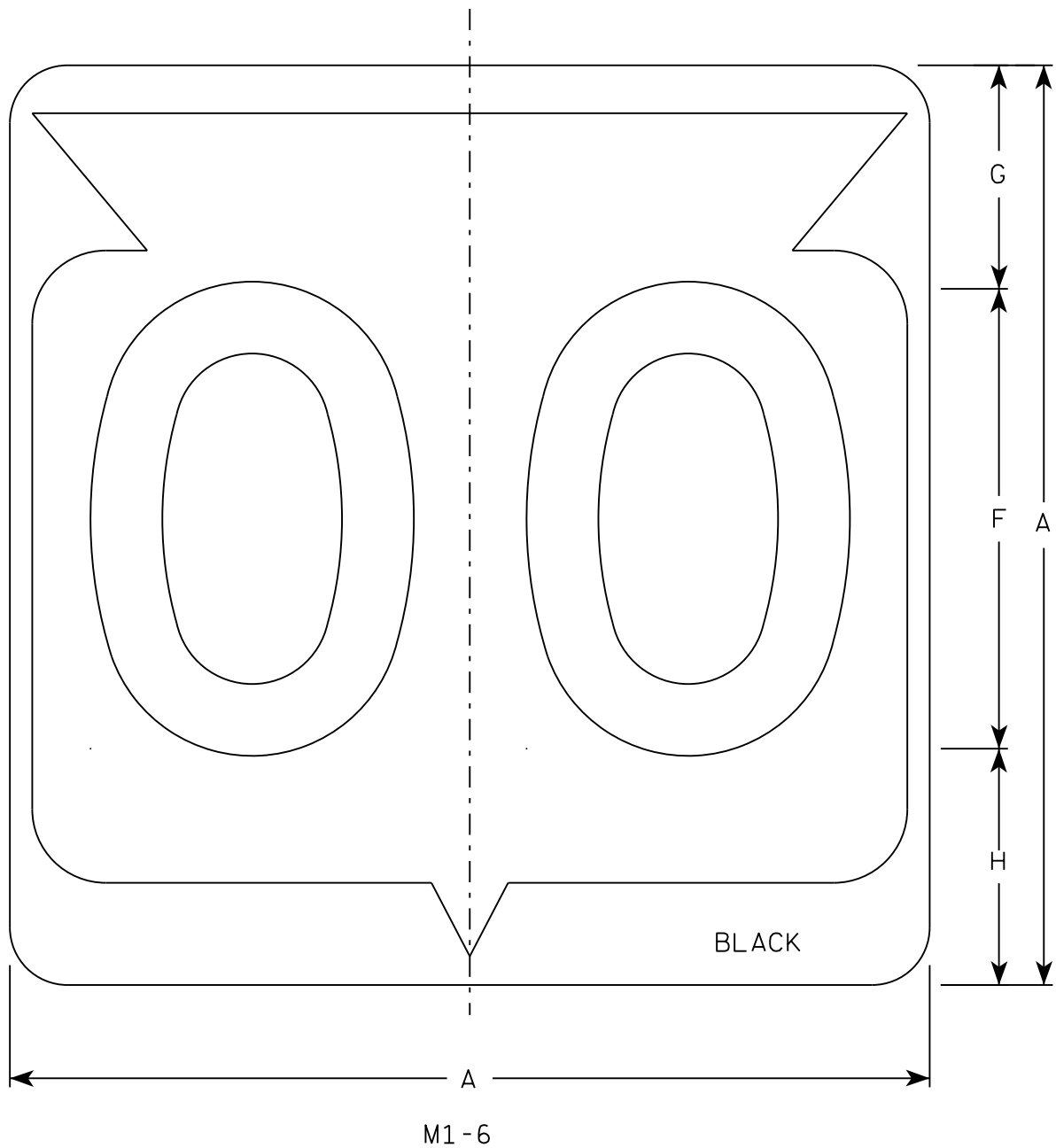
DATE 6/10/19 PLATE NO. A5-10.2

PROJECT NO:

SHEET NO:

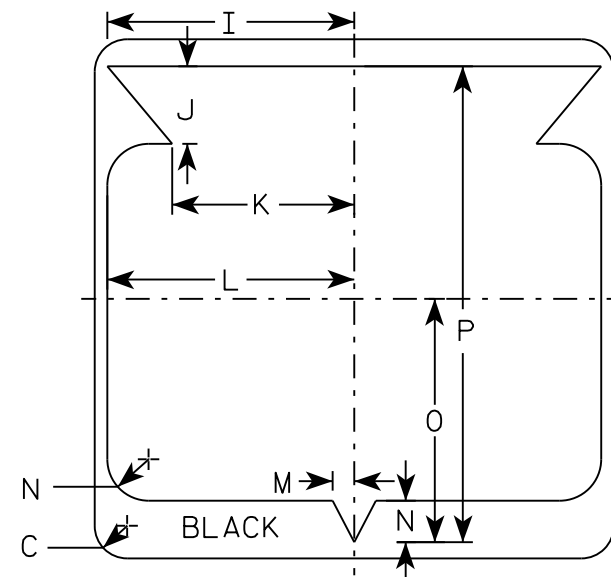
E





NOTES

1. Sign is Type II - Type H Reflective
2. Color:  
Background - White  
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

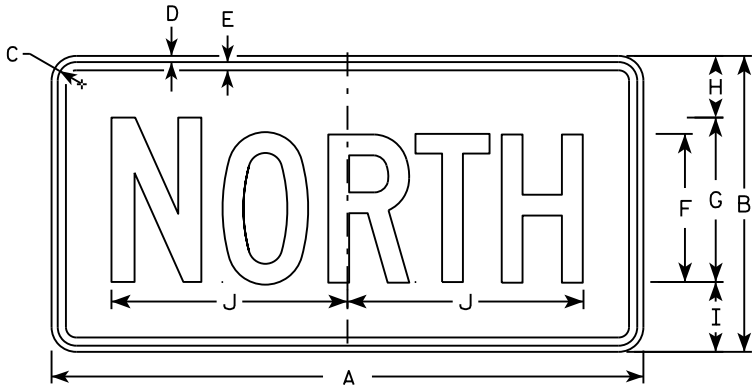
STATE ROUTE MARKER  
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-6.10

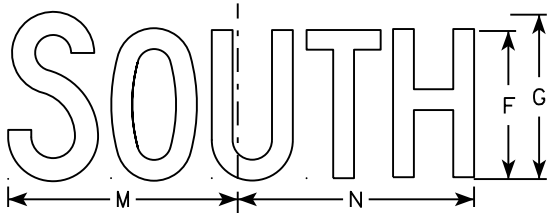
PROJECT NO: HWY: COUNTY: SHEET NO: E



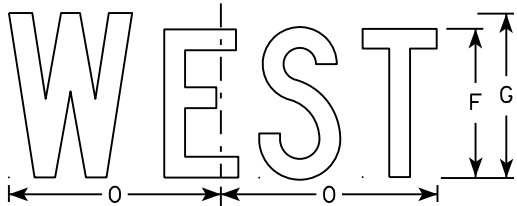
M3-1  
MM3-1  
MP3-1



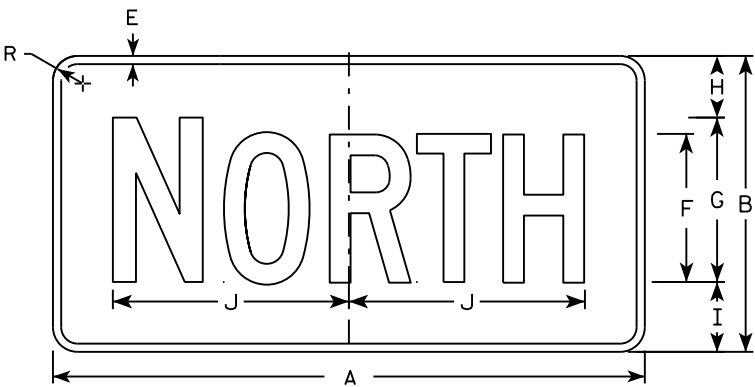
M3-2  
MM3-2  
MP3-2



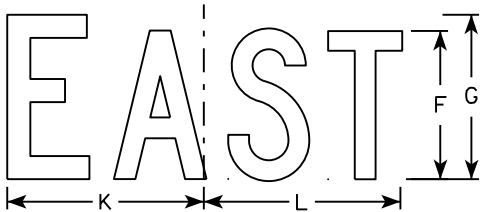
M3-3  
MM3-3  
MP3-3



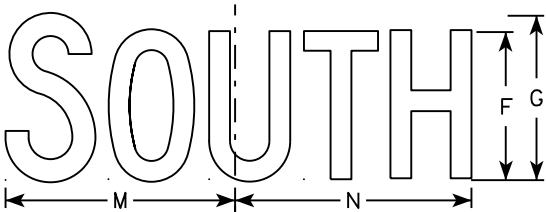
M3-4  
MM3-4  
MP3-4



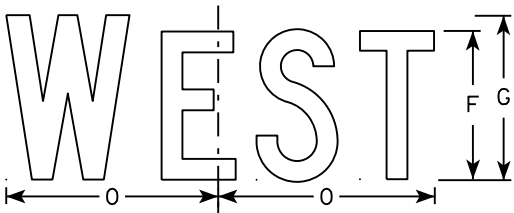
MB3-1  
MK3-1  
MN3-1



MB3-2  
MK3-2  
MN3-2



MB3-3  
MK3-3  
MN3-3



MB3-4  
MK3-4  
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:  
Background - See note 5  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White  
Message - Black  
MB3-1 thru MB3-4 Background - Blue  
Message - White  
MK3-1 thru MK3-4 Background - Green  
Message - White  
MM3-1 thru MM3-4 Background - White  
Message - Green  
MN3-1 thru MN3-4 Background - Brown  
Message - White  
MP3-1 thru MP3-4 Background - White  
Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

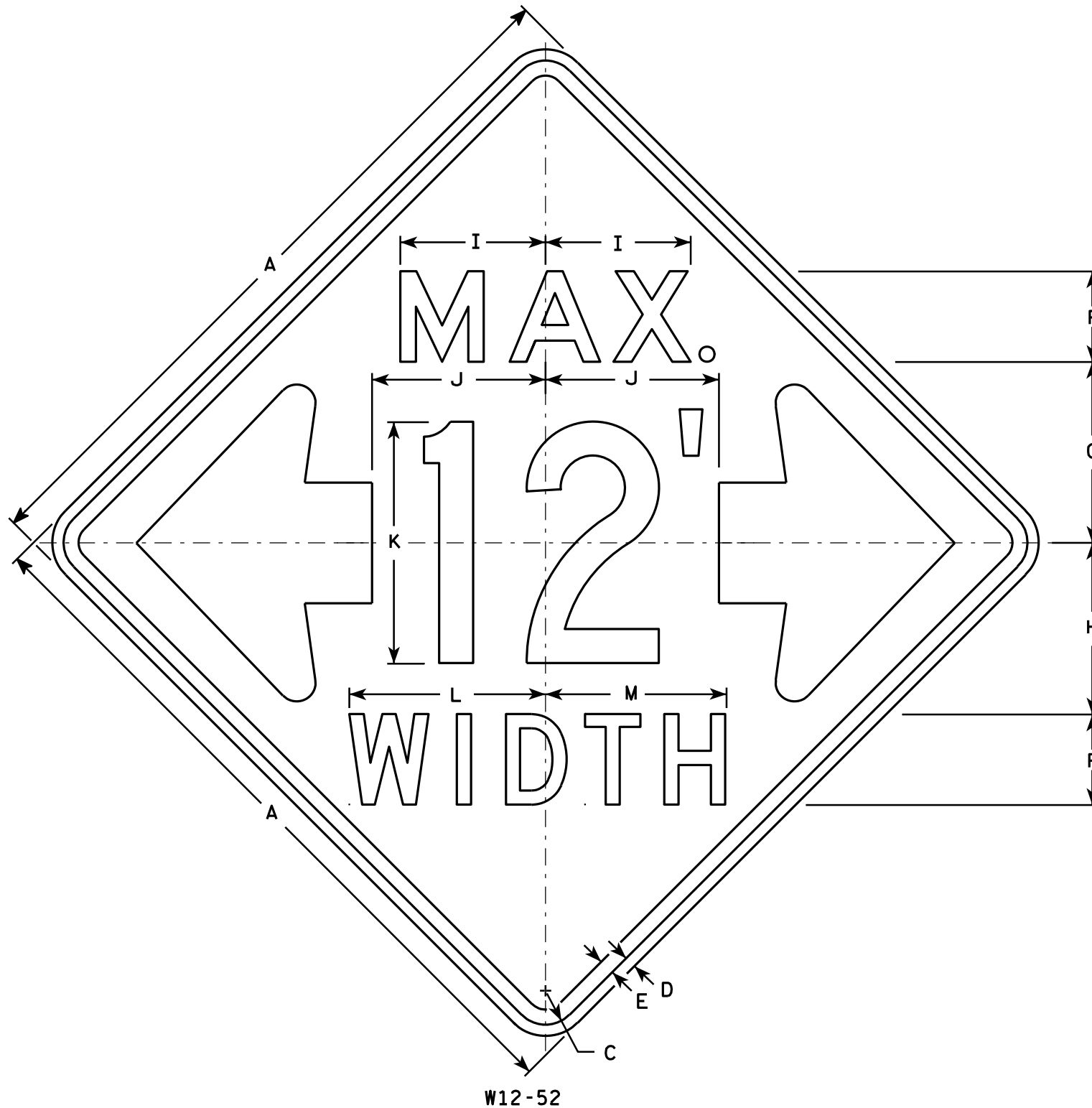
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS  
M3-1 thru M3-4  
SERIES

WISCONSIN DEPT OF TRANSPORTATION

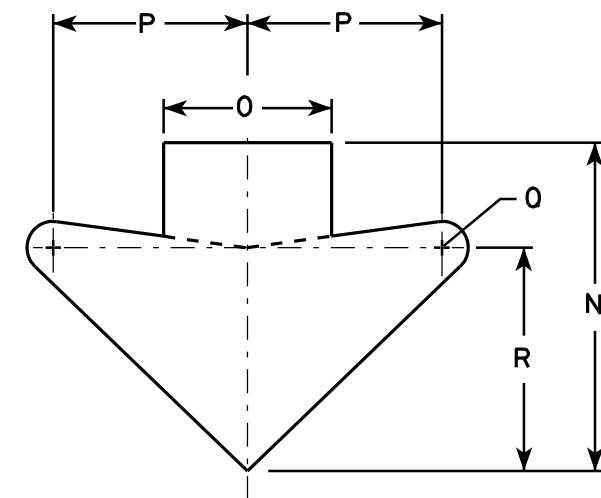
APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14



# NOTES

- Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:  
Background - Orange  
Message - Black
- Message Series - See note 5
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- The top line is series E, the numerals are series C, and the bottom line is series D.
- Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8									16.0
2M	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8									16.0
3																											
4																											
5																											

## STANDARD SIGN W12-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
For State Traffic Engineer

DATE 3/16/11 PLATE NO. W12-52.7

PROJECT NO:

HWY:

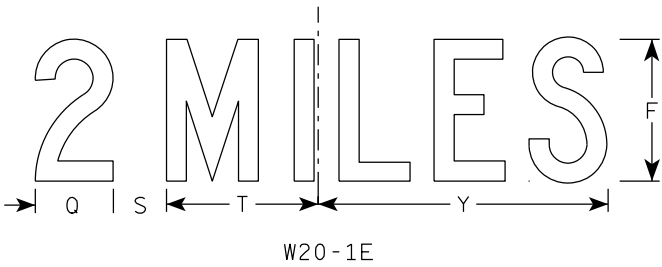
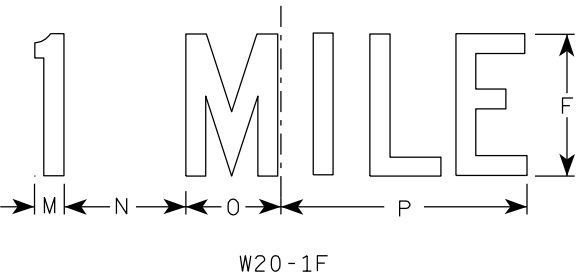
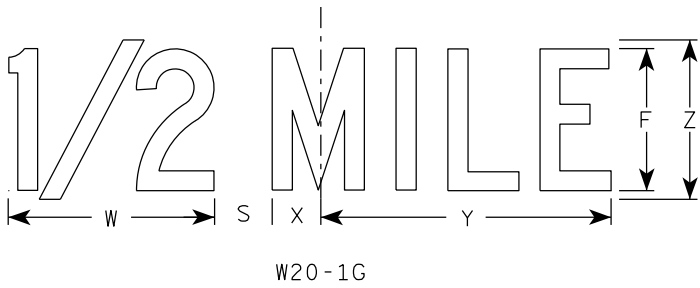
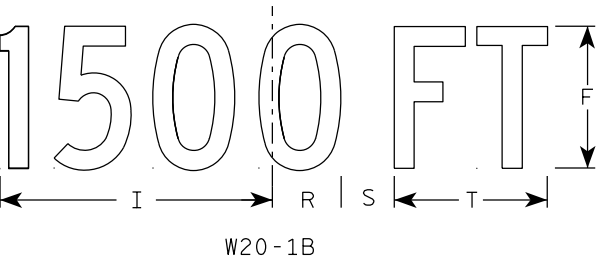
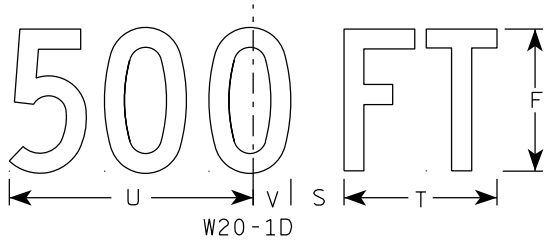
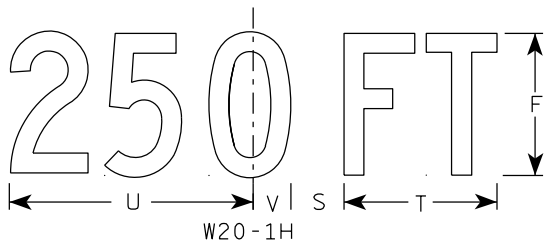
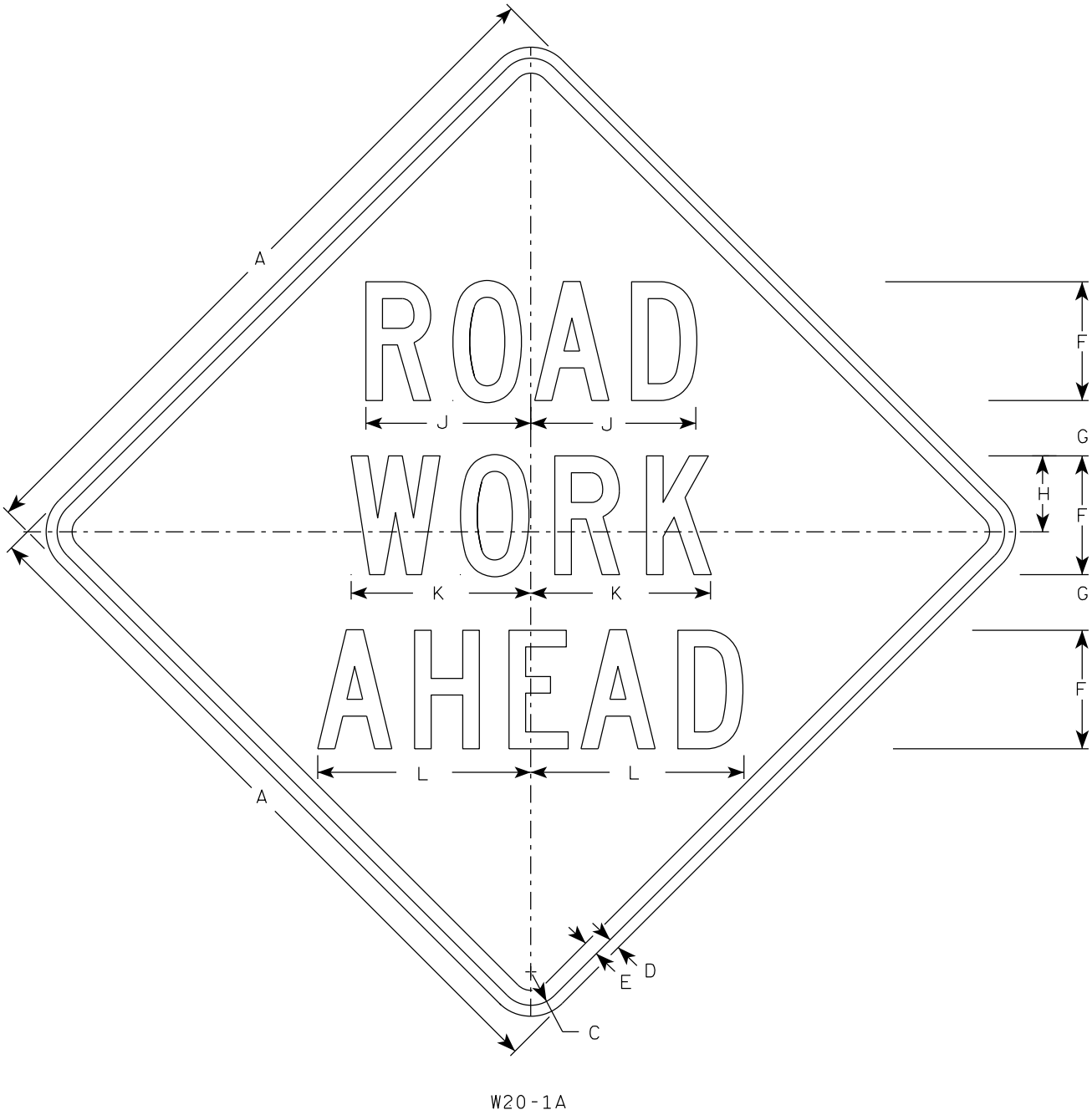
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:  
Background - Orange  
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



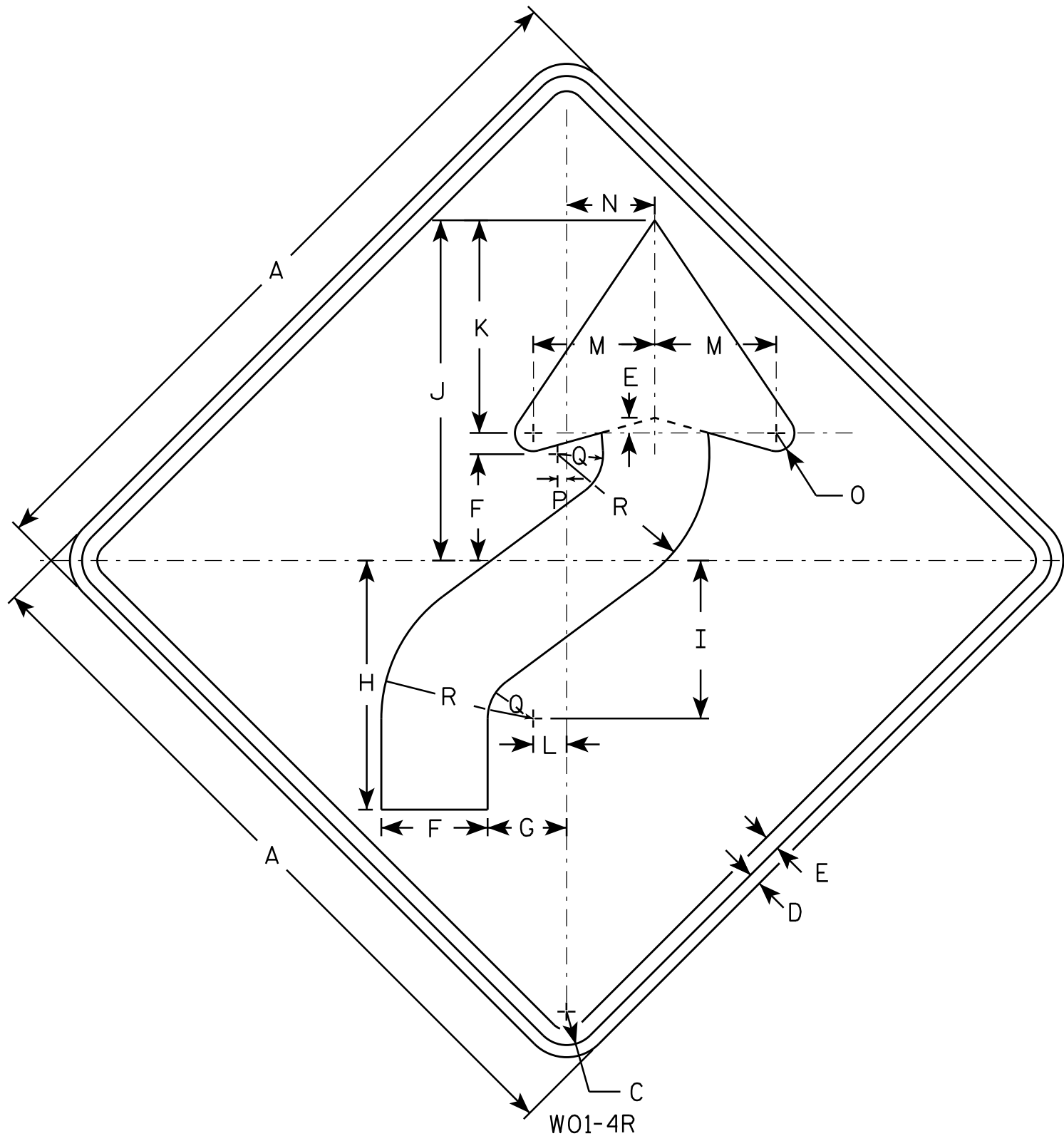
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	A <sub>req</sub> sq. ft.
1	36		1 5/8	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN  
W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/25/2020 PLATE NO. W20-1.11



### NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

## STANDARD SIGN W01-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-4.1

PROJECT NO:

HWY:

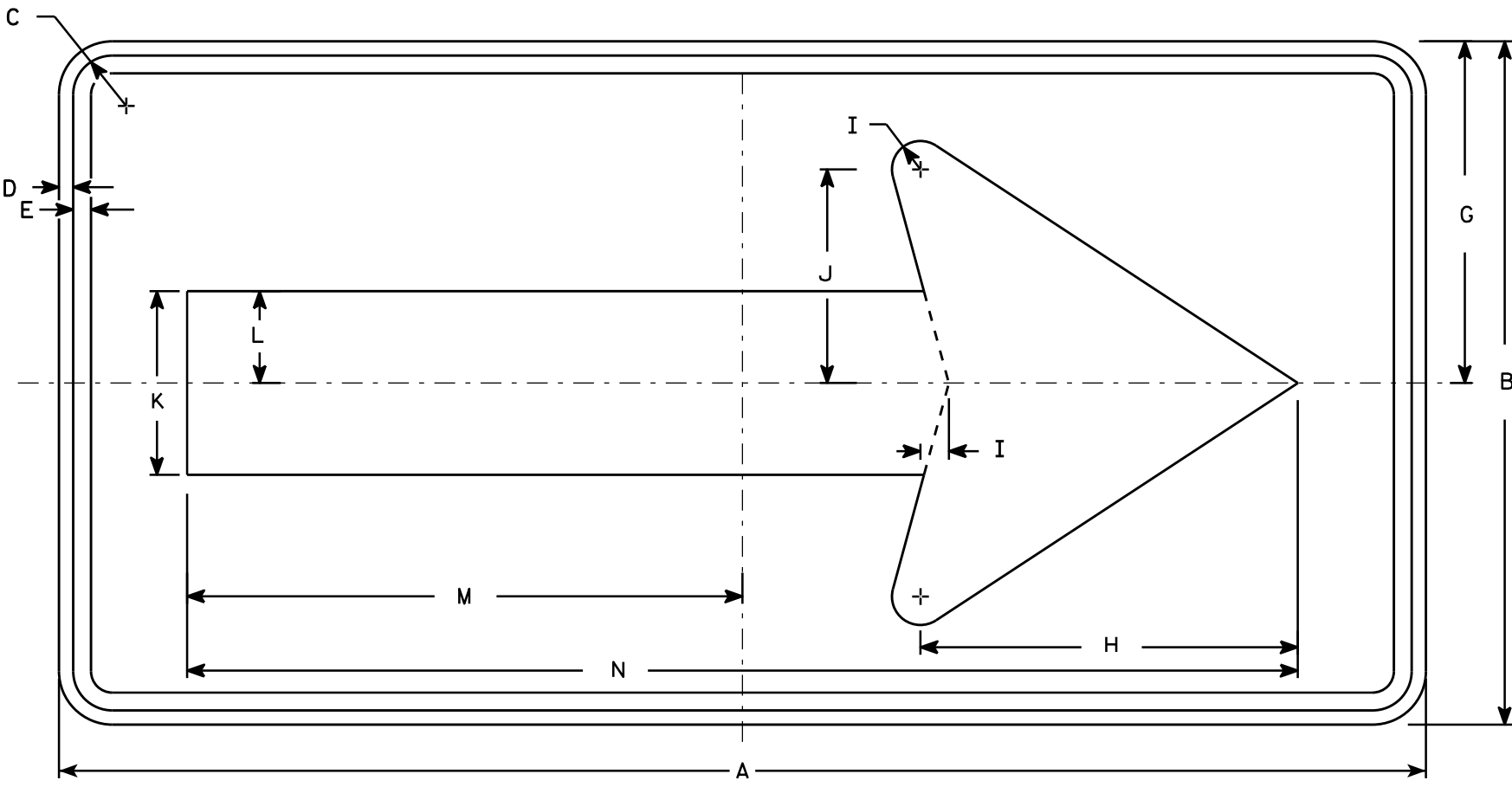
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type F Reflective - reference  
WIS DOT Standard Specification for HIGHWAY  
and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - Black
3. Corners may be square or rounded when base  
material is plywood but borders shall be rounded  
as shown. When base material is metal, the  
corners and borders shall be rounded.



W01-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
2M	48	24	1 3⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
3	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
4	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
5	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5

STANDARD SIGN

W01-6

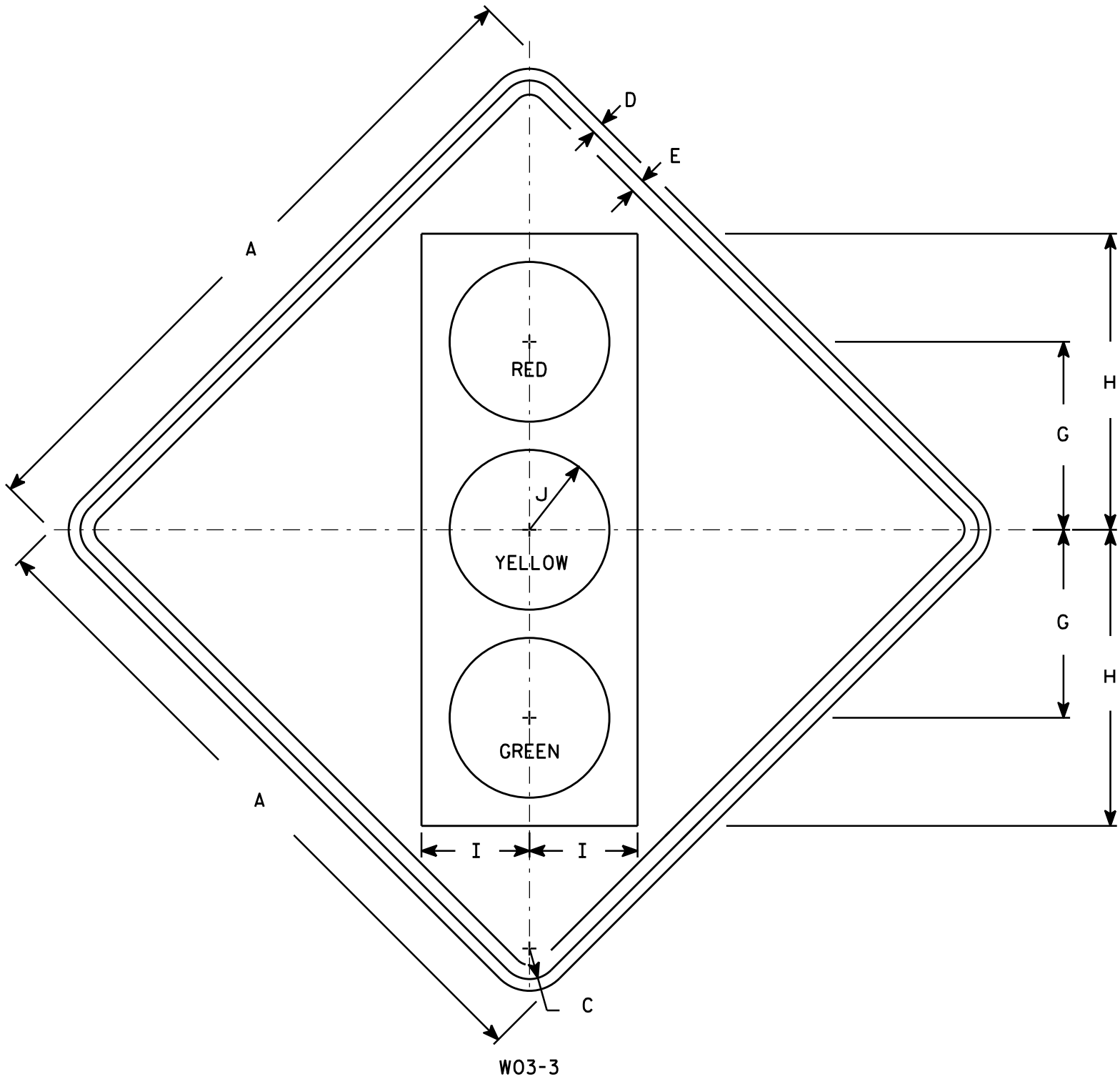
WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/18/13

PLATE NO. W01-6.1



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - See Note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Symbol and border are non-reflective black.  
Top circle - Type H Reflectorized Red  
Center circle - Same as background  
Bottom circle - Type H Reflectorized Green

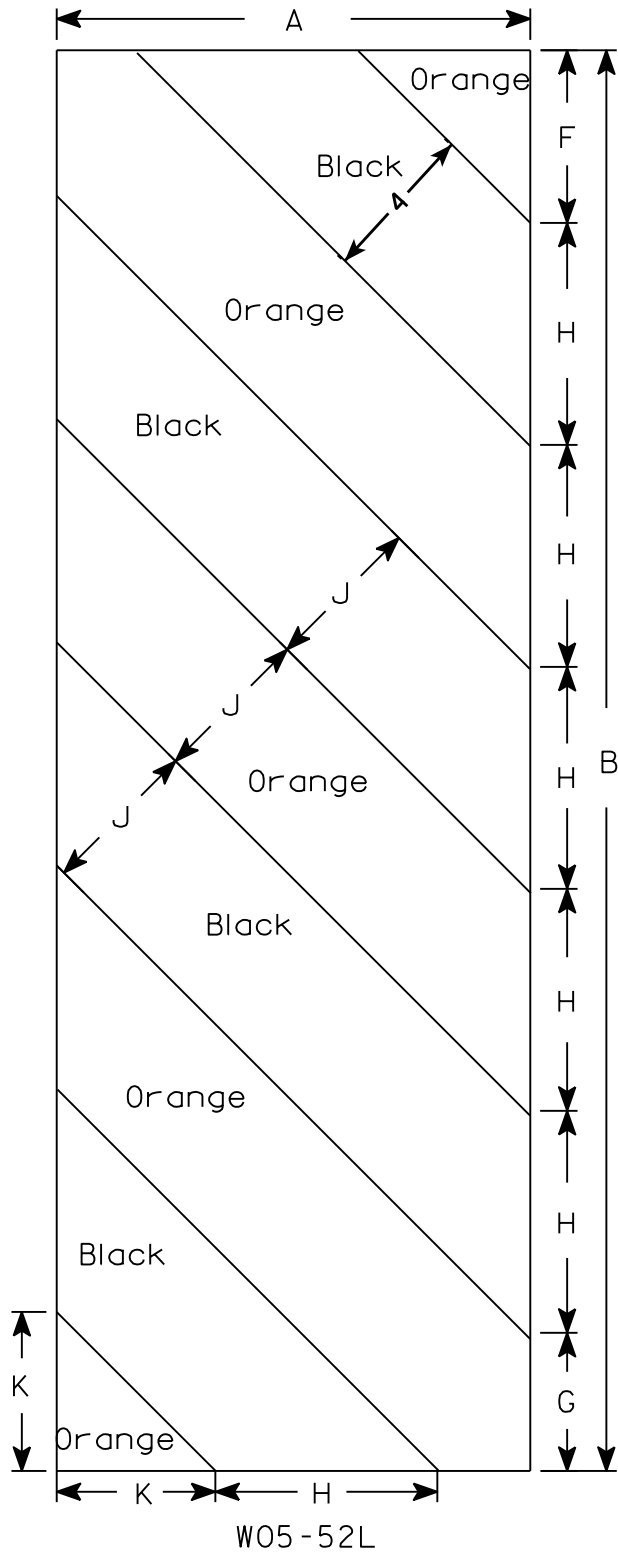
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4		10	15 3/4	5 3/4	4 1/4																	9.0
2S	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
2M	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
3	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
4	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
5	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0

STANDARD SIGN  
W03-3

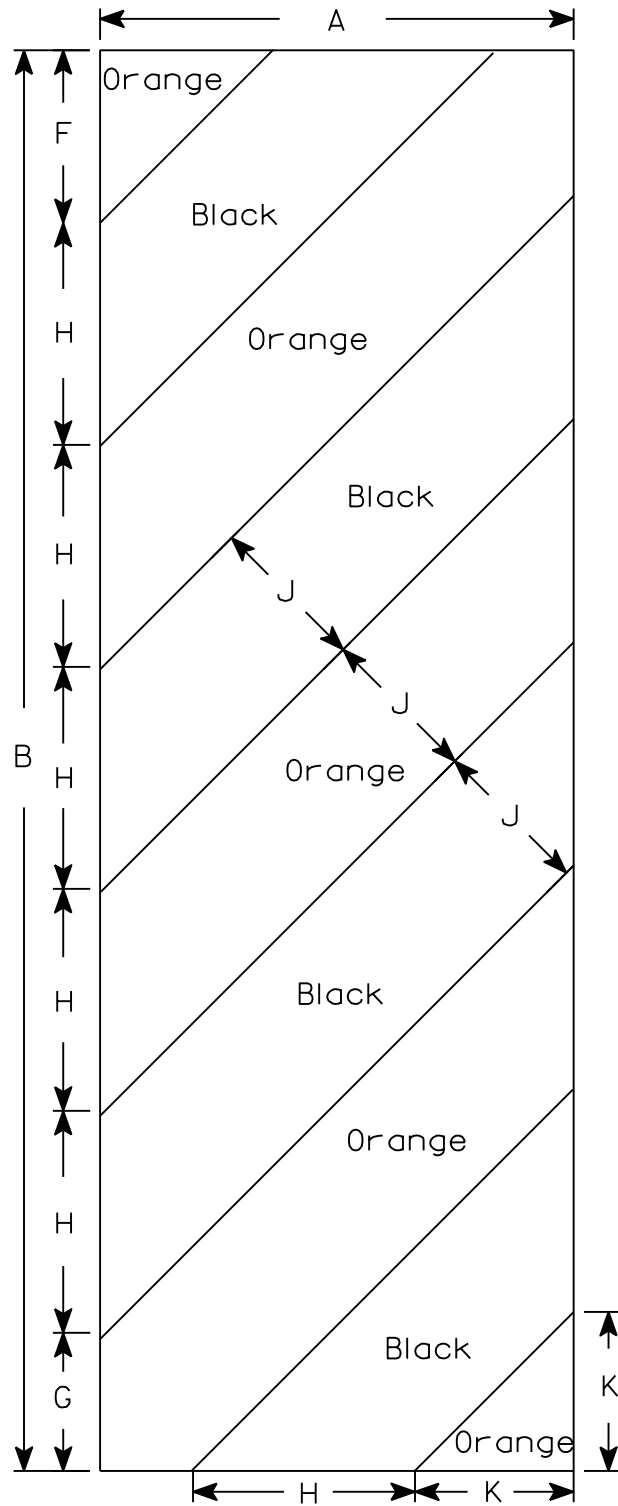
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/20/13 PLATE NO. W03-3.1



W05-52L



W05-52R

### NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3⁄8	3 1⁄2	5 5⁄8	45°	4	4																3.0
2M	12	36				4 3⁄8	3 1⁄2	5 5⁄8	45°	4	4																3.0
3	18	54				6	5 1⁄2	8 1⁄2	45°	6	6 9⁄16																6.75
4																											
5																											

STANDARD SIGN  
W05-52L & W05-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/20/13 PLATE NO. W05-52.1

PROJECT NO:

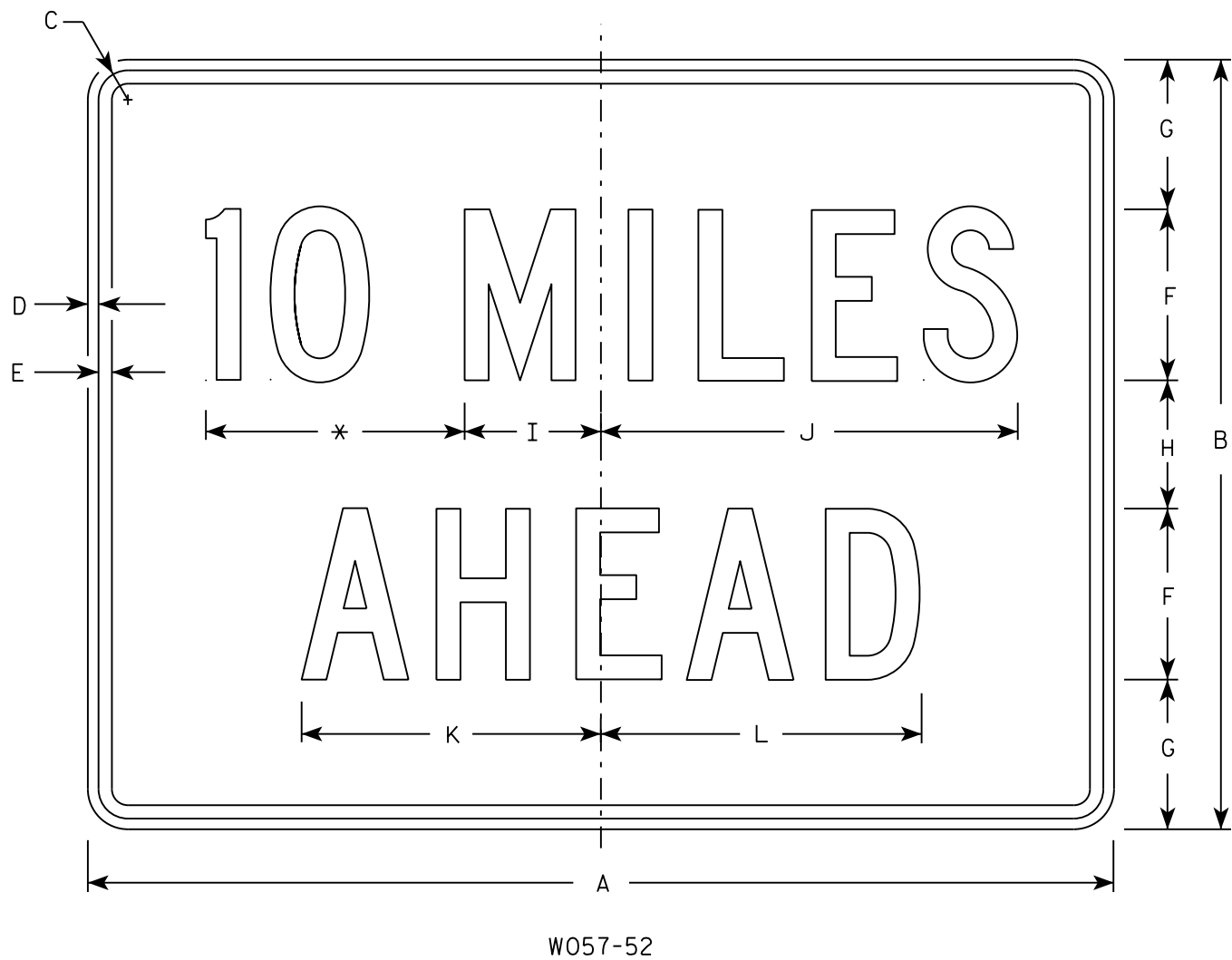
HWY:

COUNTY:

SHEET NO:

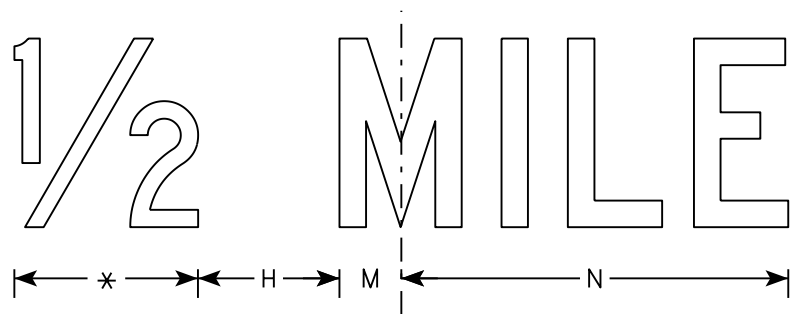
E





NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:  
Background - Orange  
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals to the nearest quarter mile and optically adjust spacing to achieve proper balance.



\* See note 5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	24	1 1/8	3/8	1/2	6	4 1/2	3	4 3/4	14 5/8	10 5/8	11 3/8	2	12													6.0
2S	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
2M	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
3	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
4	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
5	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

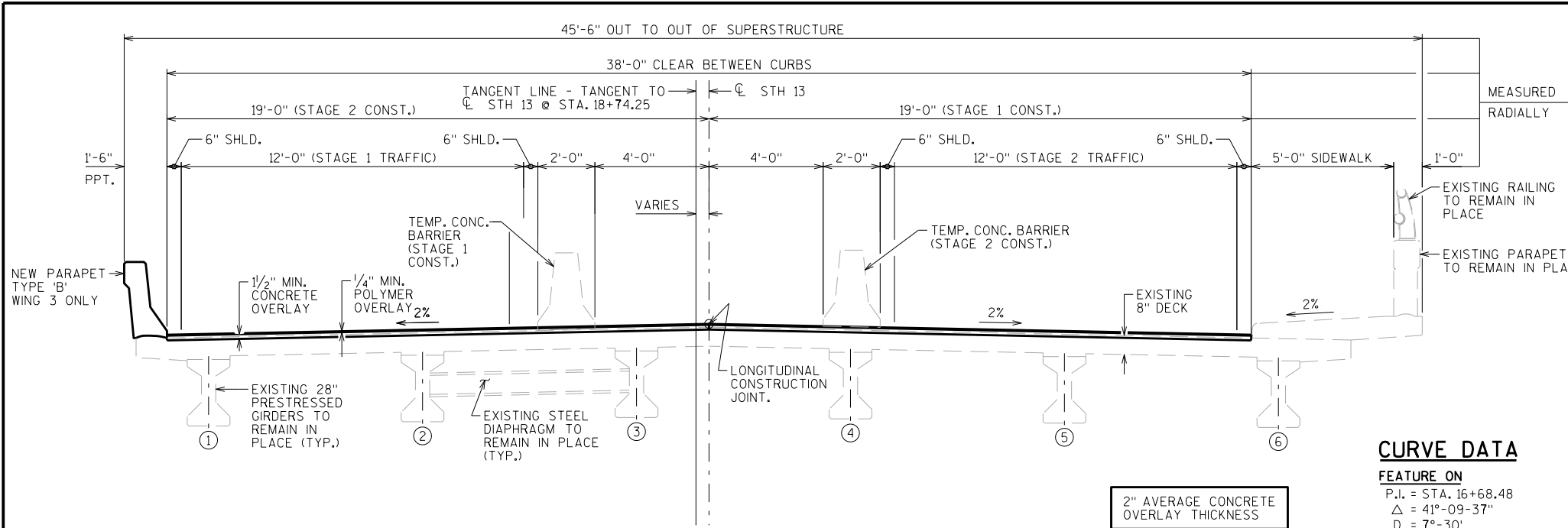
E

STANDARD SIGN  
W057-52

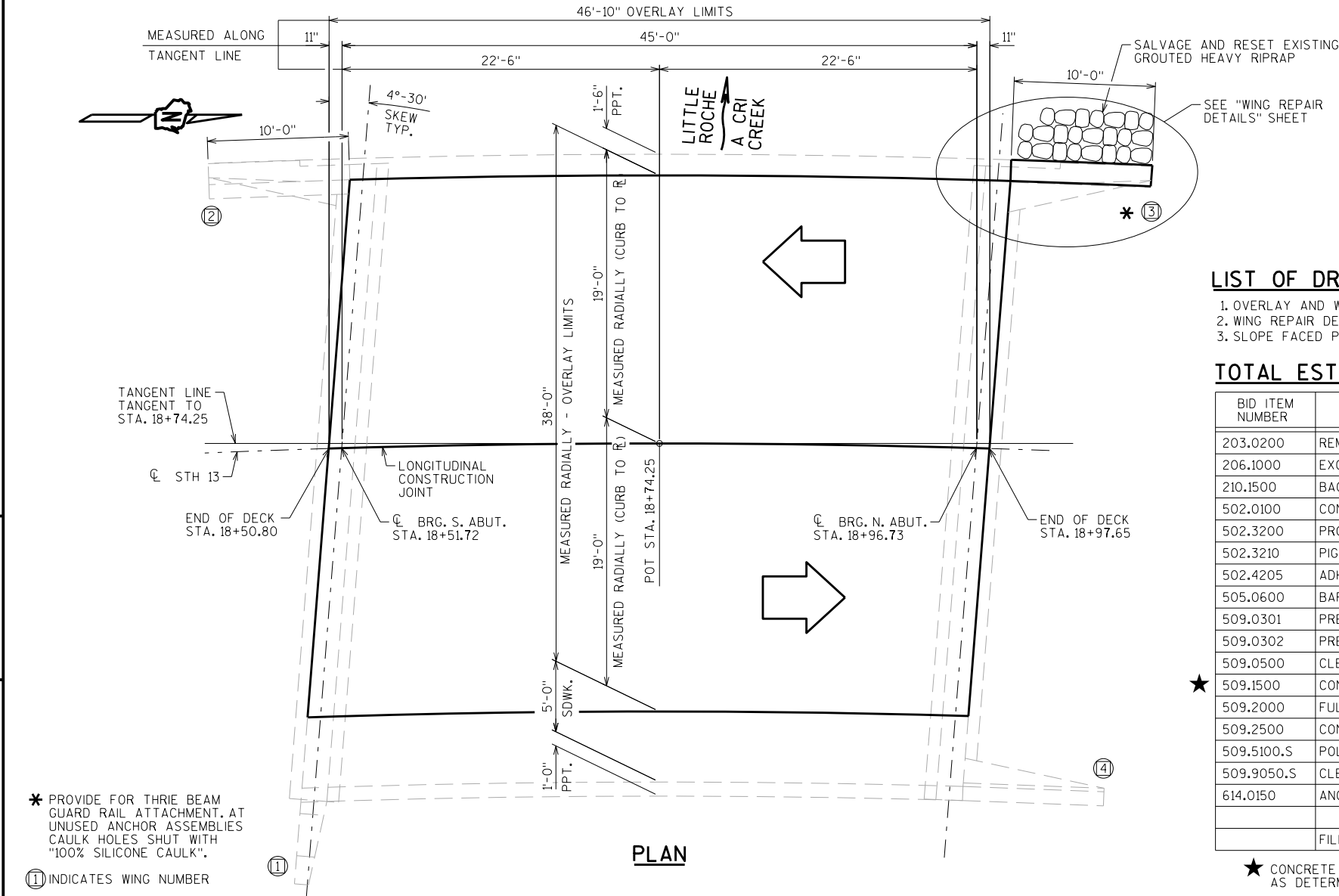
WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/21/17 PLATE NO. W057-52.2



CROSS SECTION THRU ROADWAY - LOOKING NORTH



PLAN

CURVE DATA

FEATURE ON  
P.I. = STA. 16+68.48  
Δ = 41°-09'-37"  
D = 7°-30'  
T = 286.85'  
L = 548.80'  
R = 763.94'  
S.E. = N/A

LIST OF DRAWINGS

1. OVERLAY AND WING REPAIR
2. WING REPAIR DETAILS
3. SLOPE FACED PARAPET TYPE 'B'

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
203.0200	REMOVING OLD STRUCTURE STA. 18+74.25	LS	1
206.1000	EXCAVATION FOR STRUCTURES BRIDGES B-01-17	LS	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	13
502.0100	CONCRETE MASONRY BRIDGES	CY	3
502.3200	PROTECTIVE SURFACE TREATMENT	SY	198
502.3210	PIGMENTED SURFACE SEALER	SY	25
502.4205	ADHESIVE ANCHORS NO. 5 BAR	EACH	26
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	540
509.0301	PREPARATION DECKS TYPE 1	SY	34
509.0302	PREPARATION DECKS TYPE 2	SY	14
509.0500	CLEANING DECKS	SY	198
509.1500	CONCRETE SURFACE REPAIR	SF	10
509.2000	FULL-DEPTH DECK REPAIR	SY	1
509.2500	CONCRETE MASONRY OVERLAY DECKS	CY	14
509.5100.S	POLYMER OVERLAY	SY	198
509.9050.S	CLEANING PARAPETS	LF	67
614.0150	ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD	EA	1
	NON-BID ITEMS		
	FILLER	SIZE	1/2"

★ CONCRETE SURFACE REPAIR QUANTITIES ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER.

DESIGN DATA

LIVE LOAD:  
INVENTORY RATING: HS-17  
OPERATING RATING: HS-27  
MAXIMUM STANDARD PERMIT VEHICLE LOAD: 250 (KIPS)

MATERIAL PROPERTIES:

CONCRETE MASONRY:  
OVERLAY DECKS & SUPERSTRUCTURE f'c = 4,000 P.S.I.  
ALL OTHERS f'c = 3,500 P.S.I.  
BAR STEEL REINFORCEMENT: fy = 60,000 P.S.I.  
GRADE 60

TRAFFIC VOLUME

STH 13  
ADT = 7020 (2023)  
R.D.S. = 25 M.P.H.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.  
BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.  
THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.  
BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS OTHERWISE NOTED.  
ANY EXCAVATION REQUIRED TO COMPLETE THE OVERLAY AT THE SOUTH ABUTMENT IS TO BE CONSIDERED INCIDENTAL TO THE BID ITEM "CONCRETE MASONRY OVERLAY DECKS".  
ANY EXCAVATION REQUIRED TO COMPLETE THE WING REPAIR OR THE OVERLAY AT THE NORTH ABUTMENT SHALL BE PAID FOR UNDER THE "EXCAVATION FOR STRUCTURES BRIDGES B-01-17" BID ITEM.  
BLAST CLEAN THE EXISTING PARAPET TYPE 'B', PRIOR TO APPLYING THE PIGMENTED SURFACE SEALER TO THE ENTIRE PARAPET TYPE 'B'. PAID FOR UNDER "CLEANING PARAPETS" BID ITEM.  
PIGMENTED SURFACE SEALER TO BE APPLIED TO THE FRONT FACE AND THE TOP OF THE PARAPET TYPE 'B', INCLUDING BOTH THE NEW & EXISTING PARAPET TYPE 'B'.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
PROFILE GRADE LINE SHALL BE DETERMINED IN THE FIELD BASED ON A MINIMUM CONCRETE OVERLAY THICKNESS OF 1/2" PLACED ABOVE THE DECK SURFACE AFTER SURFACE PREPARATION. EXPECTED AVERAGE CONCRETE OVERLAY THICKNESS IS 2". IF EXPECTED AVERAGE CONCRETE OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2" CONTACT THE STRUCTURES DESIGN SECTION.  
A MINIMUM OF 1-INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".  
SALVAGE AND RESET EXISTING GROUDED HEAVY RIPRAP IN THE SAME LOCATION/ CONFIGURATION AFTER THE WING REPAIR ON WING 3. PAID FOR UNDER "EXCAVATION FOR STRUCTURES BRIDGES B-01-17" BID ITEM.  
PREPARATION DECKS TYPE 1, PREPARATION DECKS TYPE 2, AND FULL-DEPTH DECK REPAIR AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION AND FULL-DEPTH DECK REPAIRS SHALL BE FILLED WITH "CONCRETE MASONRY OVERLAY DECKS".  
POLYMER OVERLAY SHALL NOT BE PLACED UNTIL A MINIMUM 28 DAY CURE HAS OCCURRED ON THE CONCRETE OVERLAY.

STRUCTURES DESIGN CONTACTS:  
AARON BONK (608) 261-0261  
MAXWELL KULICK (608) 261-6108

NO.	DATE	REVISION	BY
BUREAU OF STRUCTURES			
ACCEPTED <i>[Signature]</i> DMB 3/16/21 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-01-17			
STH 13 OVER LITTLE ROCHE A CRI CREEK			
COUNTY	ADAMS	VILLAGE	FRIENDSHIP
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	VT	DESIGNED CK'D.	MJK
DRAWN BY	DDS	PLANS CK'D.	VT
OVERLAY AND WING REPAIR			SHEET 1 OF 3

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

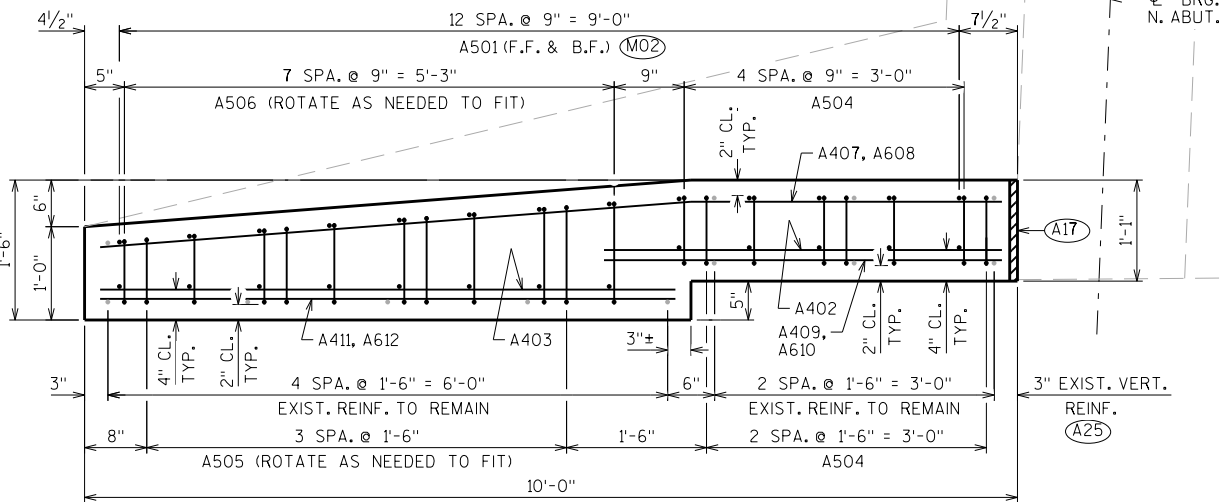
BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
A501	X	26	4'-0"			WING 3 - VERT. - MASONRY ANCH
A402	X	3	4'-0"			WING 3 - HORIZ. - TIE BARS
A403	X	3	6'-2"			WING 3 - HORIZ. - TIE BARS
A504	X	8	6'-11"	X		WING 3 - VERT.
A505	X	4	6'-2"	X		WING 3 - VERT.
A506	X	8	6'-2"	X		WING 3 - VERT.
A407	X	5	9'-8"	X		WING 3 - HORIZ. - B.F.
A608	X	1	9'-8"	X		WING 3 - HORIZ. - B.F. - TOP
A409	X	2	6'-0"			WING 3 - HORIZ. - F.F.
A610	X	1	6'-0"			WING 3 - HORIZ. - F.F. - TOP
A411	X	2	6'-2"			WING 3 - HORIZ. - F.F.
A612	X	1	6'-2"			WING 3 - HORIZ. - F.F. - TOP

▲ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

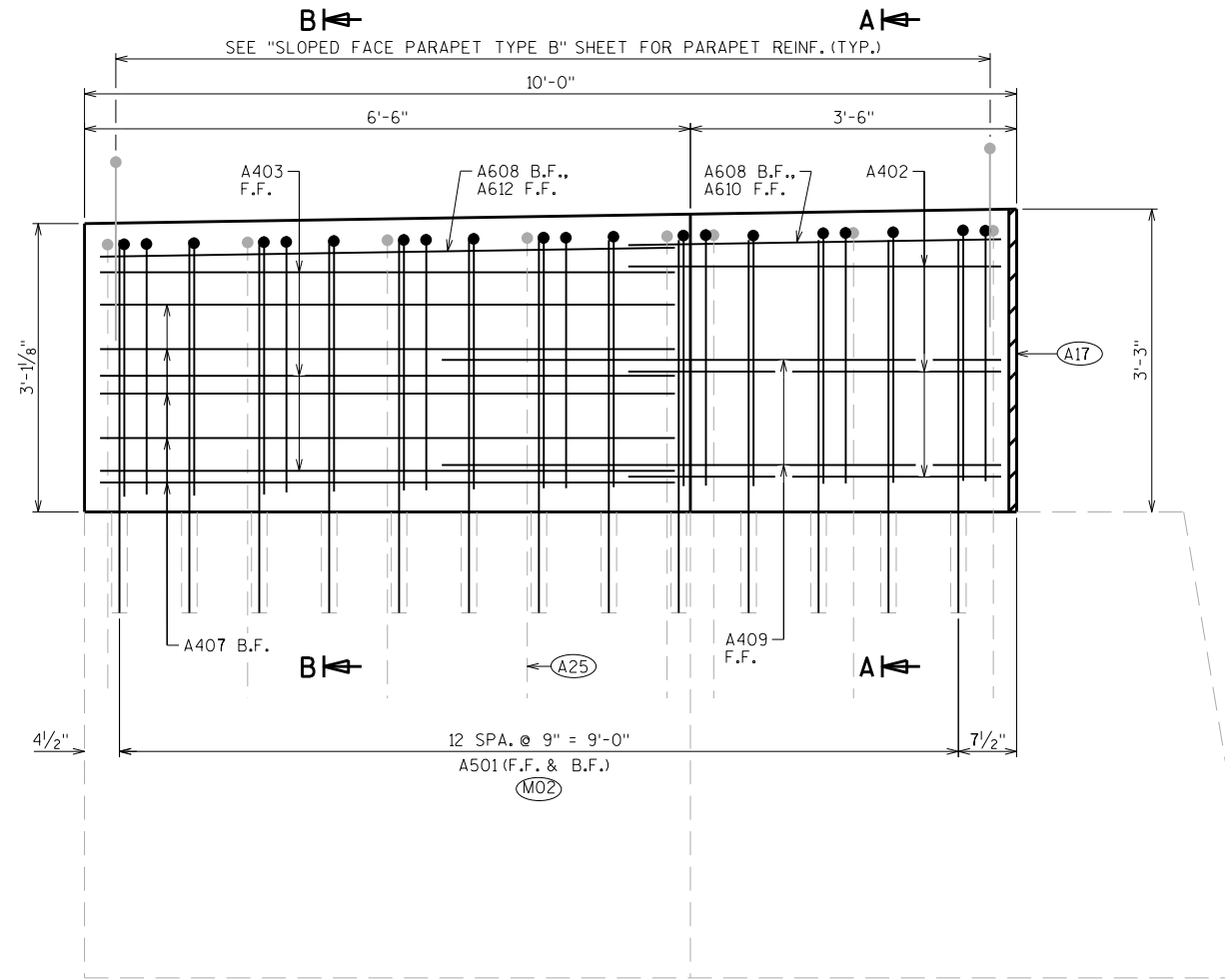
BAR SERIES TABLE

BAR MARK	NO. REQ'D.	LENGTH
A505	1 SERIES OF 4	6'-0" TO 6'-4"
A506	1 SERIES OF 8	6'-0" TO 6'-4"

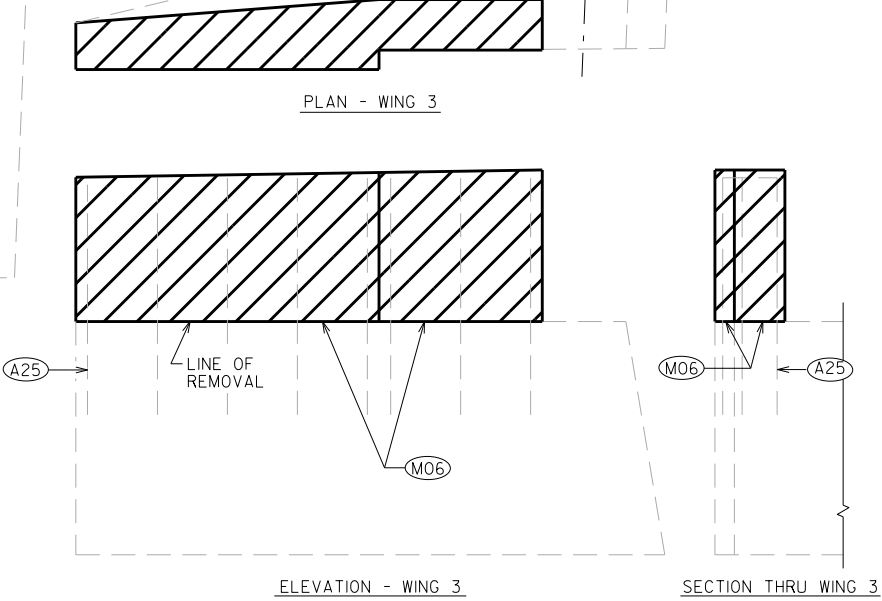
BUNDLE AND TAG EACH SERIES SEPARATELY.



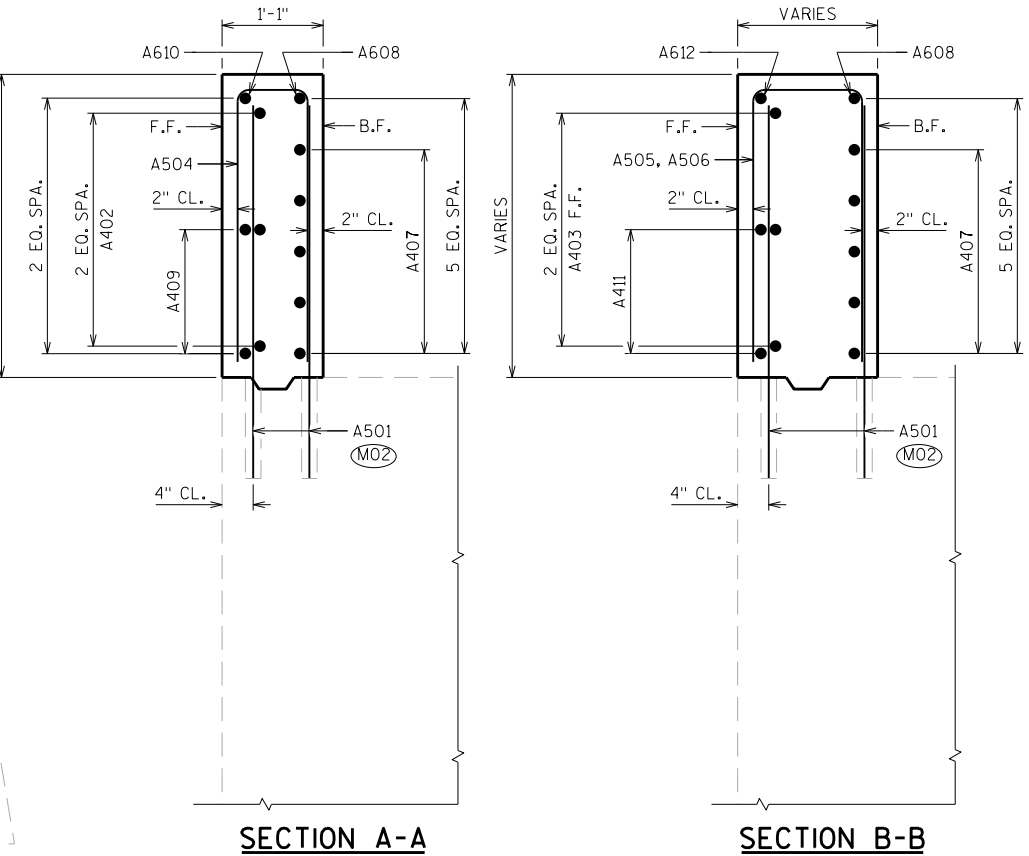
PLAN - WING 3



ELEVATION - WING 3

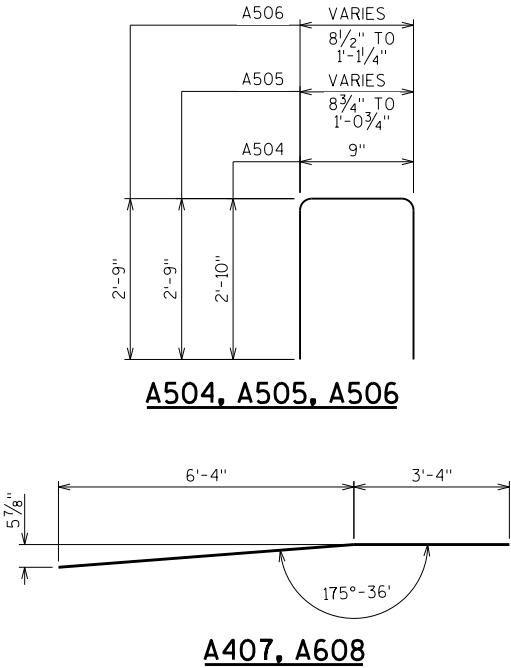


REMOVAL PLAN



SECTION A-A

SECTION B-B



A504, A505, A506

A407, A608

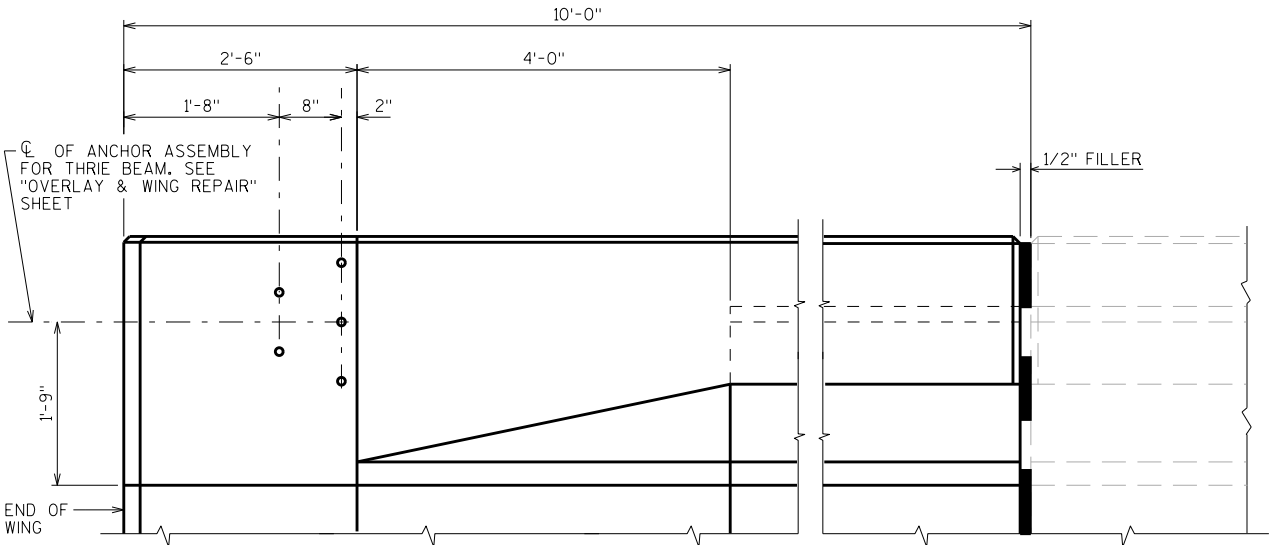
- (A17) 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER, (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- (A25) SALVAGE EXIST. VERT. REINF. & EXTEND FULL LENGTH INTO NEW WORK.
- (M02) ADHESIVE ANCHORS NO. 5 BAR, EMBED 1'-1" IN CONCRETE.
- (M06) ROUGHEN SURFACE OF CONCRETE 1/4" DEEP MINIMUM AT ALL AREAS WHERE NEW CONCRETE CONTACTS EXISTING CONCRETE.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-01-17			
DRAWN BY		DDS	PLANS CK'D. VT
WING REPAIR DETAILS		SHEET 2	

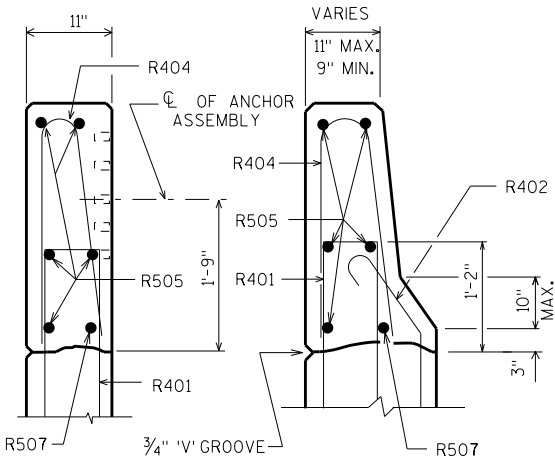
BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
R401	X	15	4'-9"	X		PARAPET - VERT. - WING 3
R402	X	4	3'-1"	X		PARAPET - VERT. - WING 3
R403	X	5	4'-9"	X		PARAPET - VERT. - WING 3
R404	X	15	4'-9"	X		PARAPET - VERT. - WING 3
R505	X	5	9'-8"			PARAPET - HORIZ. - WING 3
R406	X	5	4'-10"	X		PARAPET - VERT. - WING 3
R507	X	1	9'-8"	X		PARAPET - HORIZ. - WING 3

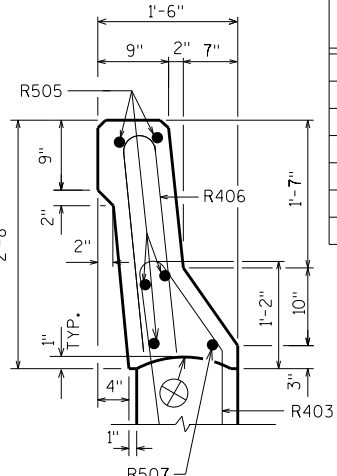


INSIDE ELEVATION

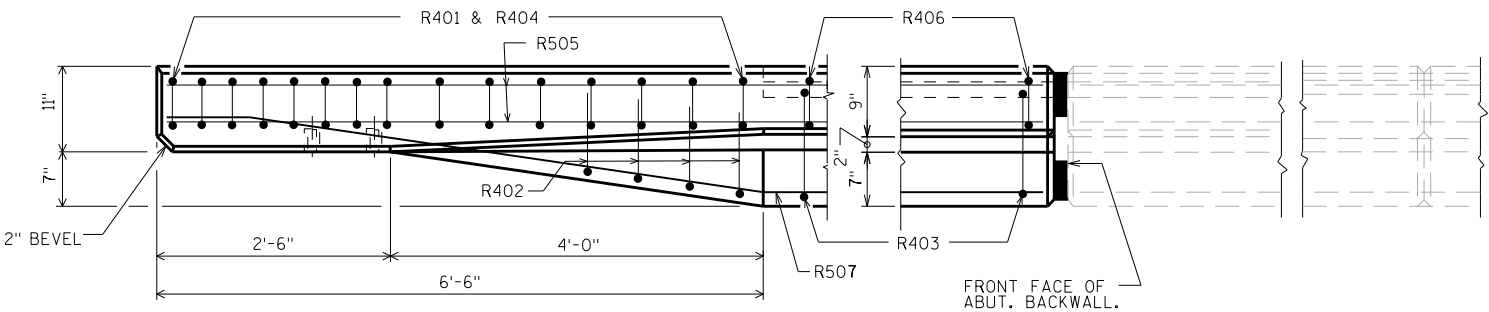


SECTION A

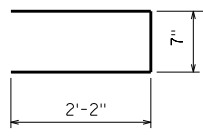
SECTION B



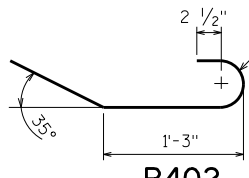
SECTION C



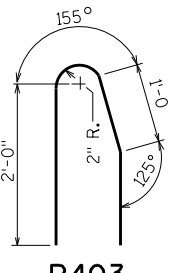
PLAN



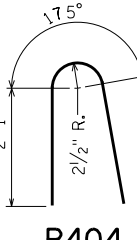
R401



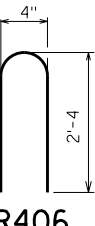
R402



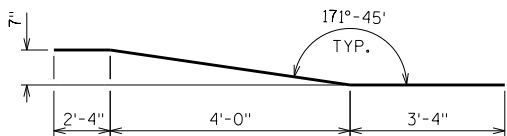
R403



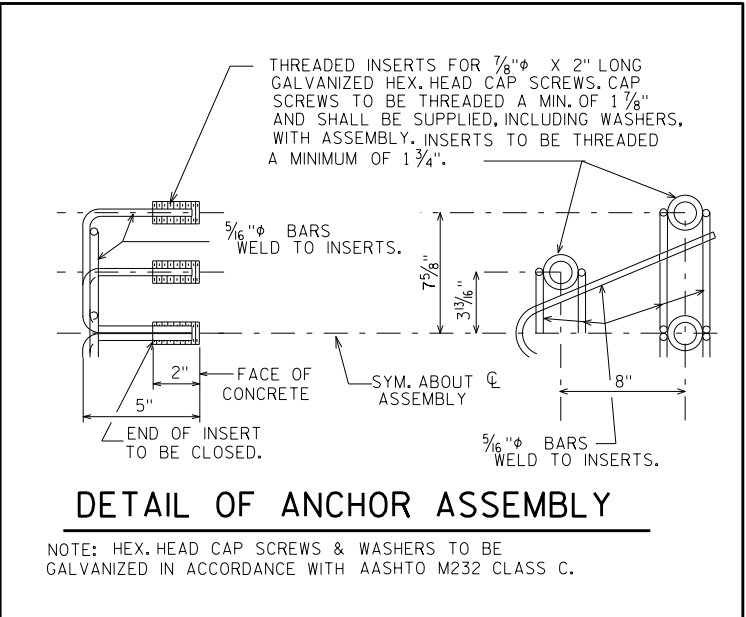
R404



R406

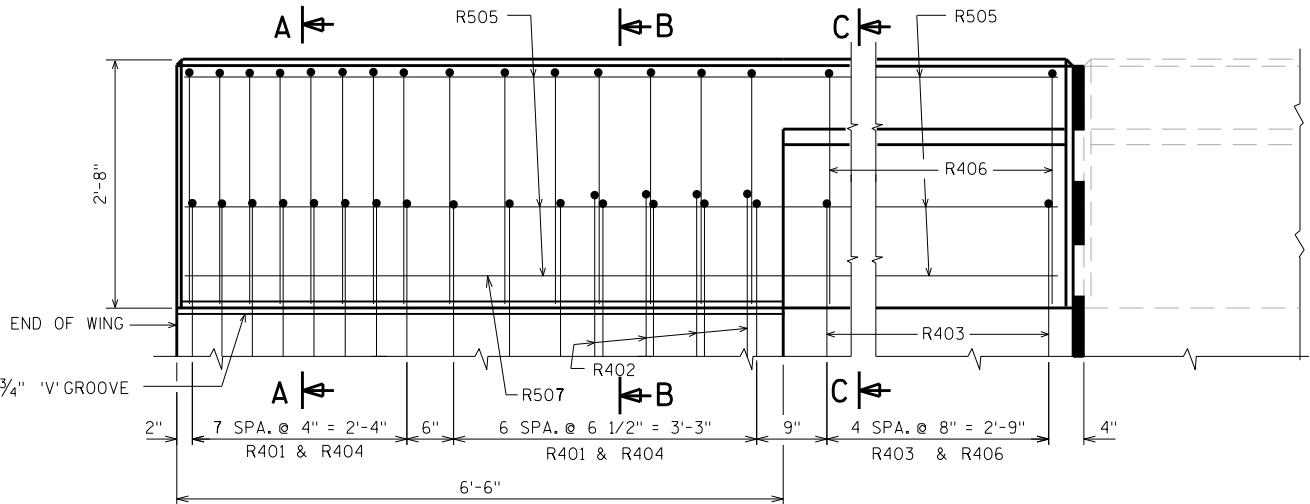


R507



DETAIL OF ANCHOR ASSEMBLY

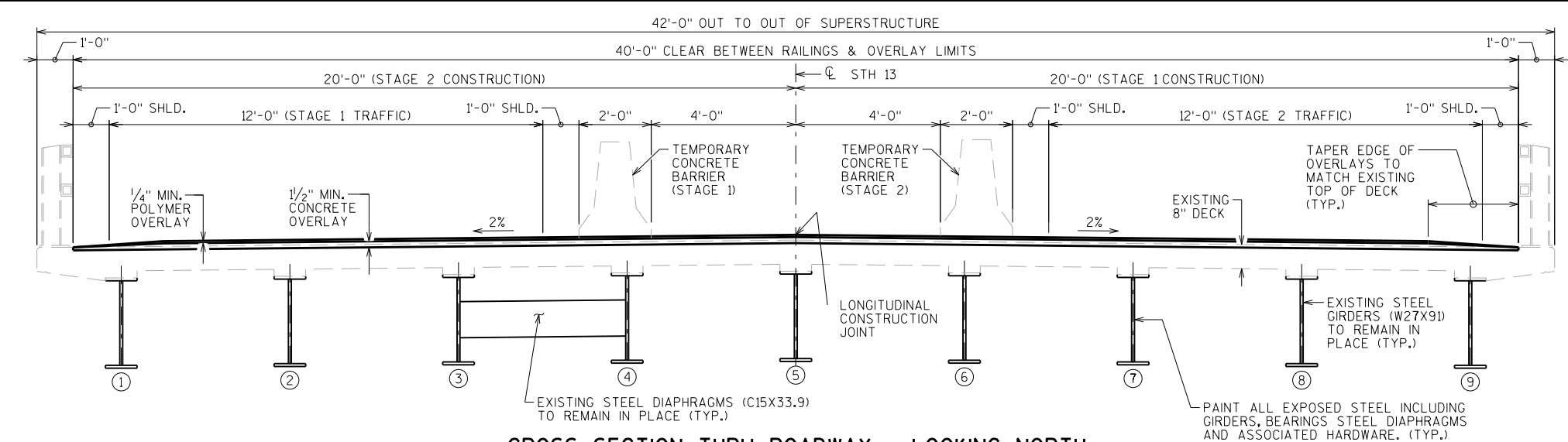
NOTE: HEX. HEAD CAP SCREWS & WASHERS TO BE GALVANIZED IN ACCORDANCE WITH AASHTO M232 CLASS C.



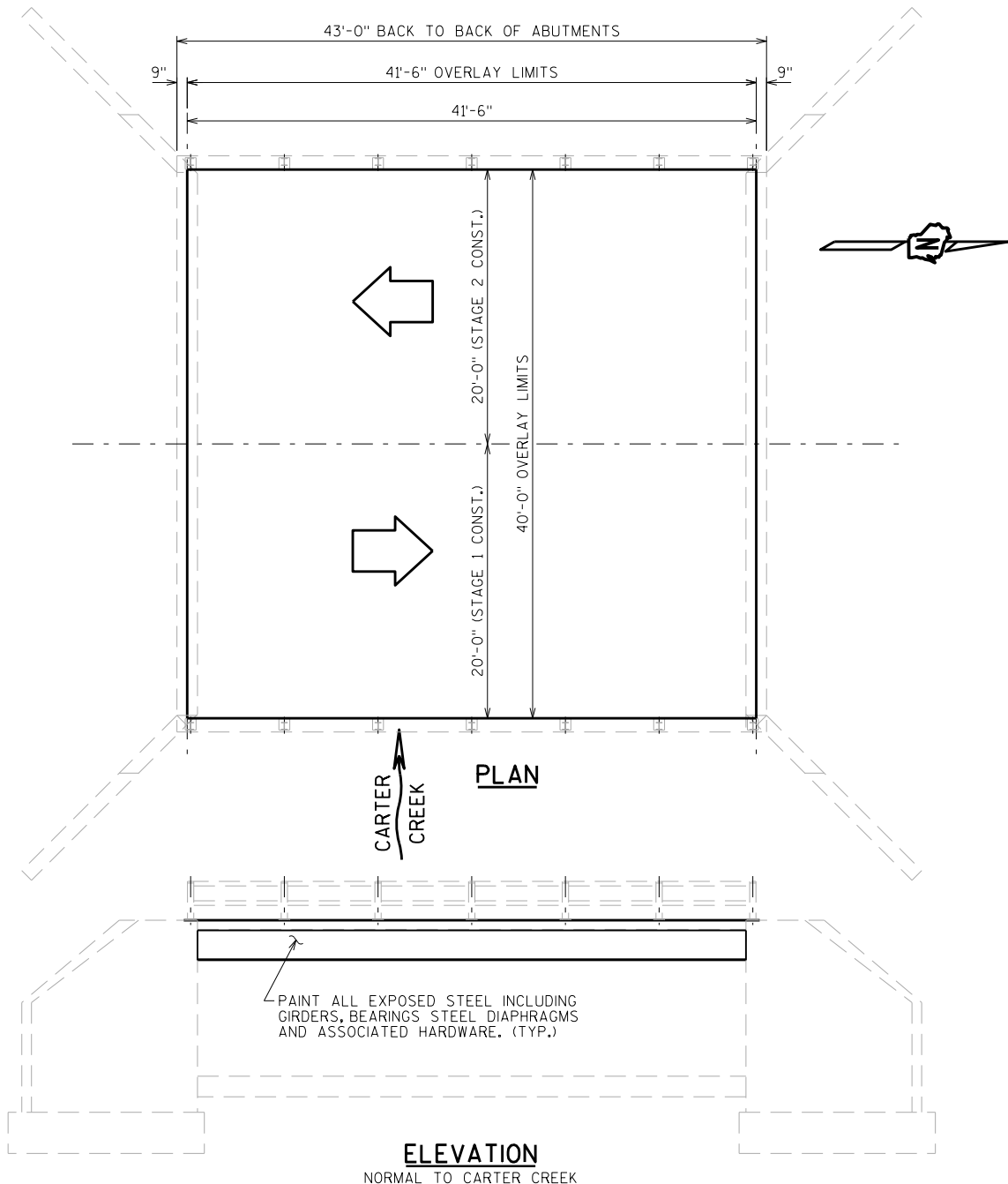
OUTSIDE ELEVATION

CONST. JOINT - STRIKE OFF AS SHOWN.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-01-17			
DRAWN BY		DDS	PLANS CK'D. VT
SLOPE FACED PARAPET TYPE 'B'		SHEET 3	



CROSS SECTION THRU ROADWAY - LOOKING NORTH



2" AVERAGE CONCRETE OVERLAY THICKNESS

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
502.3200	PROTECTIVE SURFACE TREATMENT	SY	187
509.0301	PREPARATION DECKS TYPE 1	SY	19
509.0302	PREPARATION DECKS TYPE 2	SY	9
509.0500	CLEANING DECKS	SY	187
★ 509.1500	CONCRETE SURFACE REPAIR	SF	150
509.2000	FULL-DEPTH DECK REPAIR	SY	1
509.2500	CONCRETE MASONRY OVERLAY DECKS	CY	12
509.5100.S	POLYMER OVERLAY	SY	187
517.1800.S	STRUCTURE REPAINTING RECYCLED ABRASIVE B-01-36	LS	1
517.4500.S	NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-01-36	LS	1
517.6001.S	PORTABLE DECONTAMINATION FACILITY	EACH	1

★ CONCRETE SURFACE REPAIR QUANTITIES ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER.

STATE PROJECT NUMBER

6140-01-65

DESIGN DATA

LIVE LOAD:

INVENTORY RATING: HS-20  
OPERATING RATING: HS-36  
MAXIMUM STANDARD PERMIT VEHICLE LOAD: 250 (KIPS)

MATERIAL PROPERTIES:

CONCRETE MASONRY: \_\_\_\_\_ f'c = 4,000 P.S.I.  
OVERLAY DECKS

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

PAINT ALL EXPOSED STEEL SURFACES FOR THE GIRDERS, DIAPHRAGMS AND ALL ASSOCIATED HARDWARE.

THE COLOR OF THE FINISH EPOXY TOP COAT SHALL BE LIGHT GRAY, (FEDERAL STANDARD COLOR NO. 26293) OR SIMILAR COLOR APPROVED BY THE ENGINEER.

ANY EXCAVATION REQUIRED TO COMPLETE THE OVERLAY AT THE ABUTMENTS IS TO BE CONSIDERED INCIDENTAL TO THE BID ITEM "CONCRETE MASONRY OVERLAY DECKS".

PROFILE GRADE LINE SHALL BE DETERMINED IN THE FIELD BASED ON A MINIMUM CONCRETE OVERLAY THICKNESS OF 1 1/2" PLACED ABOVE THE DECK SURFACE AFTER SURFACE PREPARATION. EXPECTED AVERAGE CONCRETE OVERLAY THICKNESS IS 2". IF EXPECTED AVERAGE CONCRETE OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2" CONTACT THE STRUCTURES DESIGN SECTION.

A MINIMUM OF 1-INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".

PREPARATION DECKS TYPE 1, PREPARATION DECKS TYPE 2, AND FULL-DEPTH DECK REPAIR AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION AND FULL-DEPTH DECK REPAIRS SHALL BE FILLED WITH "CONCRETE MASONRY OVERLAY DECKS".

POLYMER OVERLAY SHALL NOT BE PLACED UNTIL A MINIMUM 28 DAY CURE HAS OCCURRED ON THE CONCRETE OVERLAY.



TRAFFIC VOLUME

STH 13

ADT = 7,100 (2023)  
R.D.S. = 45 M.P.H.

STRUCTURES DESIGN CONTACTS:

AARON BONK (608) 261-0261  
MAXWELL KULICK (608) 261-6108

NO.	DATE	REVISION	BY
			
ACCEPTED  DMB 3/16/21 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-01-36			
STH 13 OVER CARTER CREEK			
COUNTY	ADAMS	TOWN	PRESTON
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	VT	DESIGNED CK'D.	MJK
DRAWN BY	DDS	PLANS CK'D.	VT
OVERLAY			SHEET 1 OF 1

I.D. 6140-01-35B

DATE: JULY 2018

SCALE = 6.00



## ***Wisconsin Department of Transportation***

Dedicated people creating transportation solutions  
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>