APRIL 2021

Section No.

Section No. Section No.

Section No.

Section No.

Section No.

TOTAL SHEETS =

DESIGN DESIGNATION

CONVENTIONAL SYMBOLS

= N/A

= N/A = N/A

= N/A

AADT

A.A.D.T.

D.H.V.

DESIGN SPEED

CORPORATE LIMITS

LIMITED HIGHWAY EASEMENT

PROPOSED OR NEW R/W LINE

EXISTING RIGHT OF WAY

SLOPE INTERCEPT

REFERENCE LINE

EXISTING CULVERT

PROPOSED CULVERT

COMBUSTIBLE FLUIDS

WOODED OR SHRUB AREA

MARSH AREA

PROPERTY LINE

Estimate of Quantities

Plan and Profile

Cross Sections

Miscellaneous Quantities

Standard Detail Drawings

Computer Earthwork Data

### STATE OF WISCONSIN ORDER OF SHEETS Section No. **DEPARTMENT OF TRANSPORTATION** Section No. Typical Sections and Details cludes Frosion Control Plans)

PLAN OF PROPOSED IMPROVEMENT

# C ALTOONA, USH 12

(NOTTINGHAM WAY INTERSECTON)

**USH 12 EAU CLAIRE COUNTY** 

> STATE PROJECT NUMBER 7080-00-75

R-9-W

**DEVNEY DRIVE** BRIAR LANE THE CLASSIC AT HILLCREST GREENS WINDSOR FOREST DRIVE

END PROJECT 7080-00-75 STA 9+51.65

> ORIGINAL PLANS PREPARED BY Engineering JOSHUA P. MELBY 43963-6 MADISON.

FEDERAL PROJECT

PROJECT

WISC 2021269

CONTRACT

1

STATE PROJECT

7080-00-75

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

REPARED BY Surveyor Designer

WISDOT / AEG

Project Manager

DATE: 12-04-2020 Nathan Ulness

Ε

### FILE NAME: G:\WDOTNW\19004-002 (USH 12 ALTOONA)\CIVIL 3D\SHEETSPLAN\010101-TI.DWG

GRADE LINE

ORIGINAL GROUND

SPECIAL DITCH

UTILITIES

ELECTRIC

FIBER OPTIC

SANITARY SEWER

STORM SEWER

TELEPHONE

POWER POLE

GRADE ELEVATION

CULVERT (Profile View)

MARSH OR ROCK PROFILE (To be noted as such)

<u>//////</u>

1000 FT

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN

COORDINATE REFERENCE SYSTEM (WISCRS), EAU CLAIRE COUNTY, NAD83 ( 2011 ). IN U.S. SURVEY FEET, POSITIONS SHOWN ARE GRID

COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES

ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED

TO NAVD 88 ( 2012 ). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

**BEGIN PROJECT 7080-00-75** 

₫

UTILITY PEDESTAL TELEPHONE POLE Ø

STA 0+33.23

Y = 271,735.58 X = 357,233.79

TOTAL NET LENGTH OF CENTERLINE = 0.154 MI

LAYOUT

### **UTILITY CONTACTS**

### COMMUNICATIONS

ASTREA (F/K/A/ PACKERLAND BROADBAND) RUSSELL KENNY 105 KENT STREET IRON MOUNTAIN, MI 49801 (906) 282-6434 (MOBILE) RUSSELL.KENNY@ASTREACONNECT.COM

AT&T WISCONSIN RICK PODOLAK 304 S DEWEY ST. EAU CLAIRE, WI 54701 (715) 839-5565 (OFFICE) (715) 410-0656 (MOBILE) RP4514@ATT.COM

### CINC

CONSULTANT

**DNR LIAISON** 

WISDOT

LEAH NICOL

KL ENGINEERING, INC.

(608) 663-1218

JOSHUA MELBY, P.E.

MADISON, WI 53719

5400 KING JAMES WAY, SUITE 200

JMELBY@KLENGINEERING.COM

DEPARTMENT OF NATURAL RESOURCES

1300 WEST CLAIREMONT AVE EAU CLAIRE, WI 54701 (715) 934-9014

LEAH.NICOL@WISCONSIN.GOV

WISDOT NORTHWEST REGION

EAU CLAIRE. WI 54701 (715) 836-3914

718 W. CLAIREMONT AVENUE

NATHAN.ULNESS@DOT.WI.GOV

NATE ULNESS

DAREN BAUER 105 GARFIELD AVENUE EAU CLAIRE, WI 54701 (715) 836-5286 BAUFRDP@UWFC.FDU

SPECTRUM SHANE YODER 1201 MCCANN DRIVE ALTOONA. WI 54720 (715) 214-1175 SHANE.YODER@CHARTER.COM

### ELECTRICITY

EAU CLAIRE ENERGY COOPERATIVE PAT BETHKE P.O. BOX 368 FALL CREEK, WI 54742 (715) 832-1603 PBETHKE@ECEC.COM

ALTOONA MUN WATER & SEWER UTILITY SCOTT KWICK 1303 LYNN AVENUE ALTOONA, WI 54720 (715) 829-6693 SKWICK@CI.ALTOONA.WI.US

### GAS/PETROLEUM

XCEL ENERGY BRADY GARDOW P.O. BOX 8 EAU CLAIRE, WI 54702-0008 (715) 737-1450 (OFFICE) (715) 577-0445 (MOBILE) BRADY.P.GARDOW@XCELENERGY.COM

### WATER

SEWER

ALTOONA MUN WATER & SEWER UTILITY SCOTT KWICK 1303 LYNN AVENUE ALTOONA, WI 54720 (715) 829-6693 SKWICK@CI.ALTOONA.WI.US



### HMA PAVEMENT, WHEN INDICATED ON THE PLANS, SHALL CONSIST OF COURSE THICKNESSES AS FOLLOWS:

ROADWAY	TOTAL PAVEMENT THICKNESS	TYPE
PATH	2.0"	ASPHALTIC SURFACE

STANDARD ABBREVIATIONS BASE AGGREGATE DENSE BENCH MARK RM BLDG. BUILDING CTR CENTER C/L CENTERLINE COMMERCIAL ENTRANCE C.E. CONC. CONCRETE CSW CONCRETE SIDEWALK CMCP CORRUGATED METAL CULVERT PIPE CP CULVERT PIPE CPCS **CULVERT PIPE CORRUGATED STEEL** CPRC CULVERT PIPE REINFORCED CONCRETE CPRCHE CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL CPT CONSTRUCTION PERMIT FR **EASTBOUND** ELEC **ELECTRIC** INVERT ELEVATION EX. **EXISTING** FO FIBER OPTIC

HMA HOT MIX ASPHALT HSE. HOUSE LHE LEFT HAND FORWARD MH MANHOLE MAX MAXIMUM MIN MINIMUM NOR. NORMAL NTS NOT TO SCALE

GAS

F.E.

GAS

PIF PERMANENT LIMITED EASEMENT P.E. PRIVATE ENTRANCE P.L PROPERTY LINE PROP PROPOSED PRW PROPOSED RIGHT-OF-WAY

FIELD ENTRANCE

RAD **RADIUS** R/L REFERENCE LINE

**RCCP** REINFORCED CONCRETE CULVERT PIPE REQ'D. REQUIRED RHF RIGHT HAND FORWARD

RW RIGHT-OF-WAY LINE SAN SANITARY SEWER SHLD SHOULDER SW SIDEWALK

SF SQUARE FEET SY SQUARE YARD

S.D.D. STANDARD DETAIL DRAWING STA STATION

SS STORM SEWER TEL

**TELEPHONE** TLE TEMPORARY LIMITED EASEMENT TYP

TYPICAL WAT WATER WB WESTBOUND

### **GENERAL NOTES**

ALL RADII DIMENSIONS ARE MEASURED TO THE FACE OF CURB UNLESS OTHERWISE NOTED.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN, THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE. ANY LOCAL, MUNICIPAL, OR OTHER UTILITY THAT IS NOT A MEMBER A DIGGERS HOTLINE SHALL BE CONTACTED SEPARATELY.

RIGHT OF WAY LINES SHOWN ON THE PLANS ARE APPROXIMATE.

NO TREES OR SHRUBS ARE TO THE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

THE EXACT LOCATIONS OF PRIVATE ENTRANCES ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER. ALL DRIVEWAYS ARE TO REPLACED IN KIND UNLESS OTHERWISE DIRECTED BY THE ENGINEER, OR AS SHOWN ON

SIDEWALK AND CURB & GUTTER REPLACEMENT SHOULD BE TO THE NEAREST JOINT. LIMITS ARE APPROXIMATE AND ARE TO BE VERIFIED IN THE FIELD BY THE ENGINEER. MATCH EXISTING SIDEWALK WIDTH.

PROTECT INLETS WITH PROPER INLET PROTECTION AT LOCATIONS EXHIBITING RISK OF BEING IMPACTED BY CONSTRUCTION OPERATIONS AS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY THE OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

PLACE TOPSOIL IN ALL GRADED AREAS AS DESIGNATED BY THE ENGINEER IMMEDIATELY AFTER GRADING HAS BEEN COMPLETED. SEED AND FERTILIZE ALL AREAS 5 DAYS AFTER PLACEMENT OF TOPSOIL.

TEMPORARY STORAGE OF ANY EXCAVATED MATERIAL WILL NOT BE PERMITTED IN WETLANDS, FLOODWAY, OR FLOODPLAIN OF ANY WATERWAY.

SAWCUT ASPHALTIC AND CONCRETE DRIVEWAYS AND/OR PARKING LOTS AT THE MATCHLINE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

THE EROSION CONTROL FEATURES AS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER

EROSION CONTROL DEVICES SHALL BE PLACED IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

5 FOOT WIDE SIDEWALK REQUIRES TRANSVERSE JOINTS SPACED AT 5 FEET.

FOR ALL CURB RAMPS. REFER TO THE STANDARD DETAIL DRAWINGS FOR THE RAMP TAPER DIMENSIONS. SIDEWALK WIDTHS ARE DIMENSIONED IN THE PLAN

					HYDF	ROLOGIC	SOIL	GRO	DUP			
		Δ	١		В	5		(	2			)
	9	LOPE F (PERC	RANGE ENT)	9	SLOPE I (PERC	RANGE ENT)	S	LOPE F (PERC	RANGE ENT)	S	ANGE ENT)	
LAND USE:	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT						0.70 - 0.95						
CONCRETE						0.80 - 0.95						
BRICK						0.70 - 0.80						
DRIVES, WALKS						0.75 - 0.85						
ROOFS						0.75 - 0.95						
GRAVEL ROADS, SHOUL	DERS		-			0.40 - 0.60						

TOTAL PROJECT AREA = 0.98 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.63 ACRES

PROJECT NO: 7080-00-75 HWY: USH 12 FILE NAME

G:\WDOTNW\19004-002 (USH 12 ALTOONA)\CIVIL 3D\SHEETSPLAN\020201-GN.DWG LAYOUT NAME - 020201-gn

COUNTY: EAU CLAIRE PLOT DATE : 2/2/2021 1:44 PM

**GENERAL NOTES** 

KL ENGINEERING

PLOT NAME

PLOT SCALE :

SHEET 1 IN:100 FT

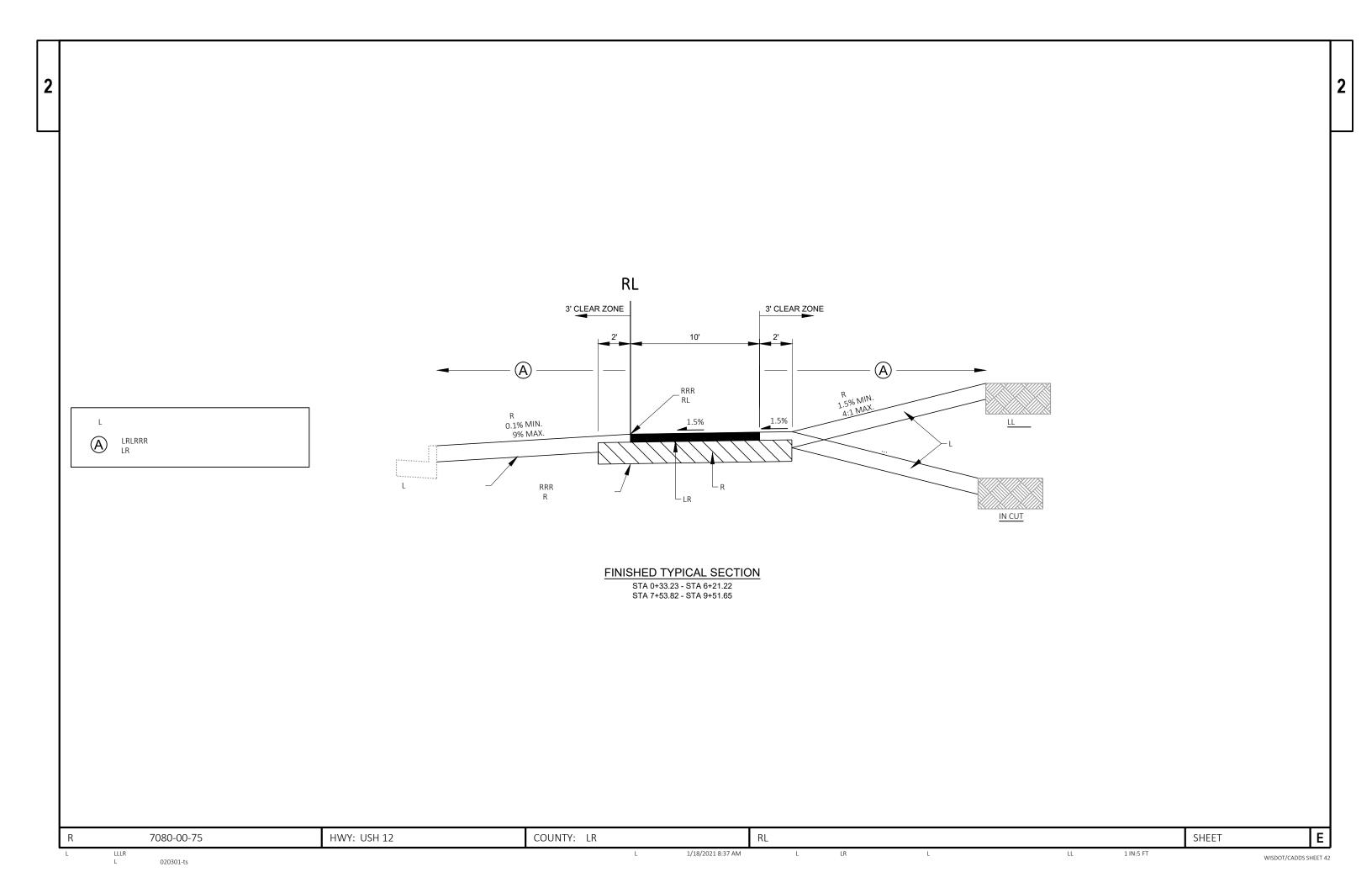
WISDOT/CADDS SHEET 42

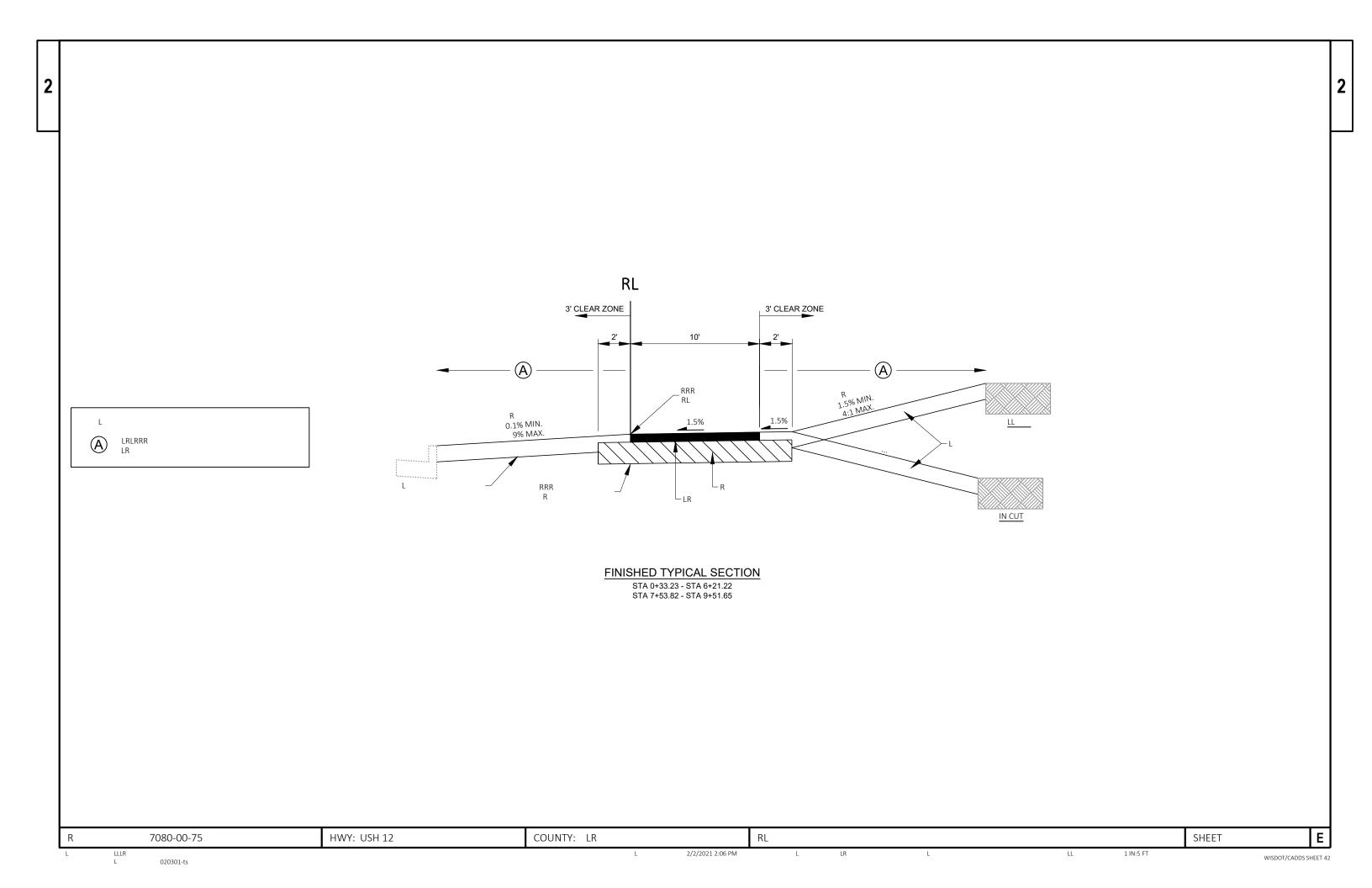
ORDER OF DETAIL SHEETS

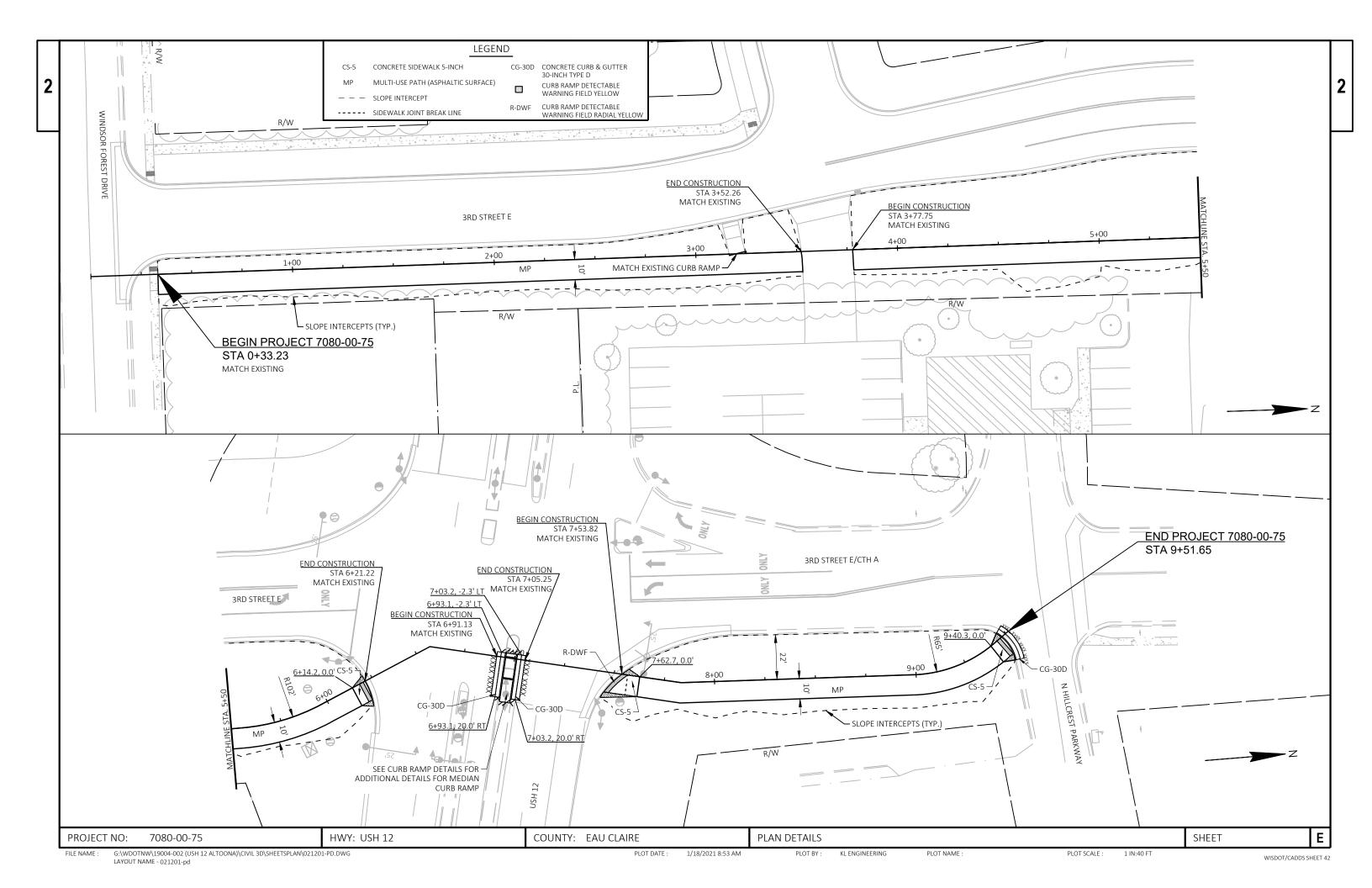
PROJECT OVERVIEW TYPICAL SECTIONS CONSTRUCTION DETAILS PLAN DETAILS **EROSION CONTROL** TRAFFIC SIGNAL PAVEMENT MARKING & SIGNING

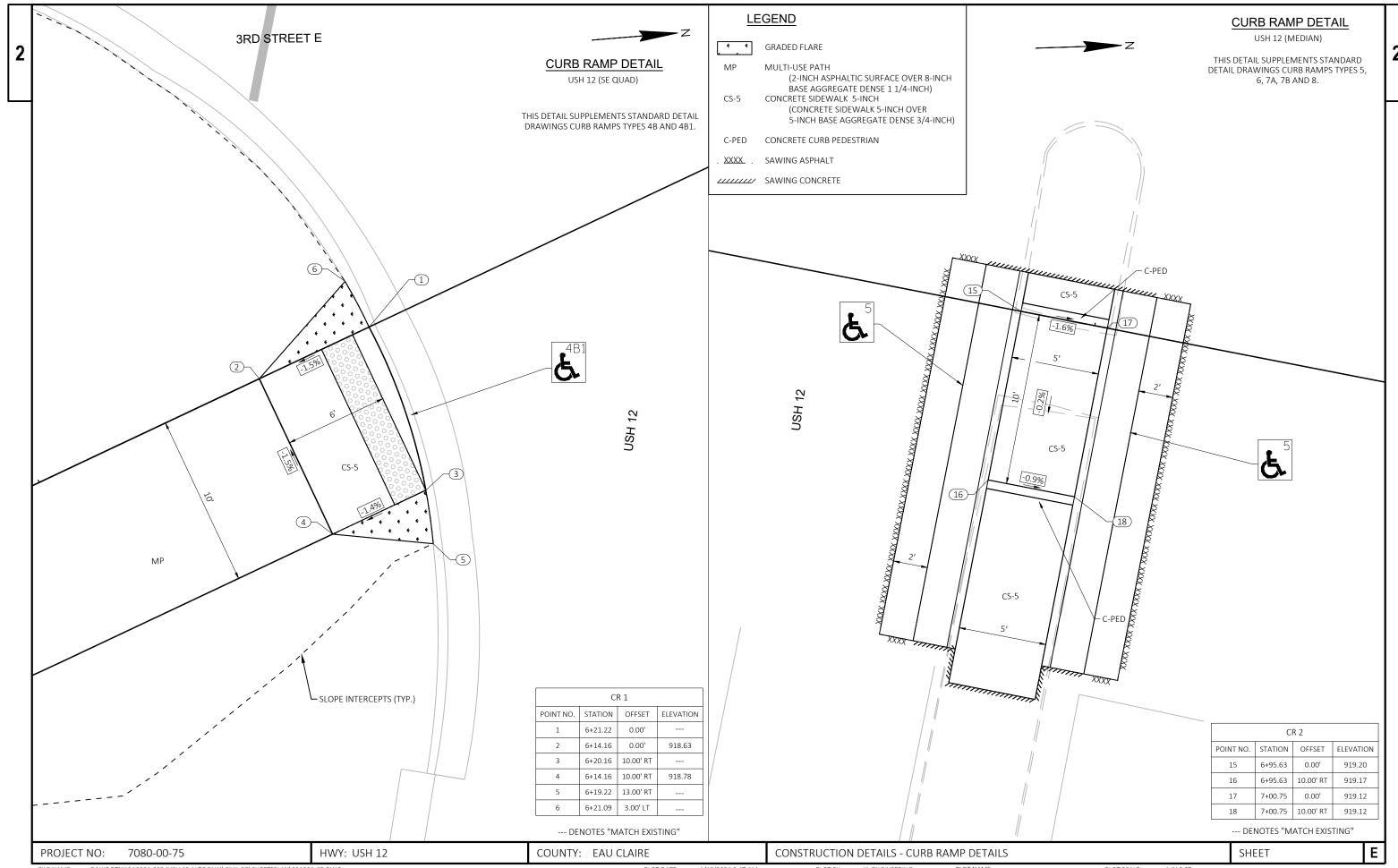
TRAFFIC CONTROL & CONSTRUCTION STAGING PLAN



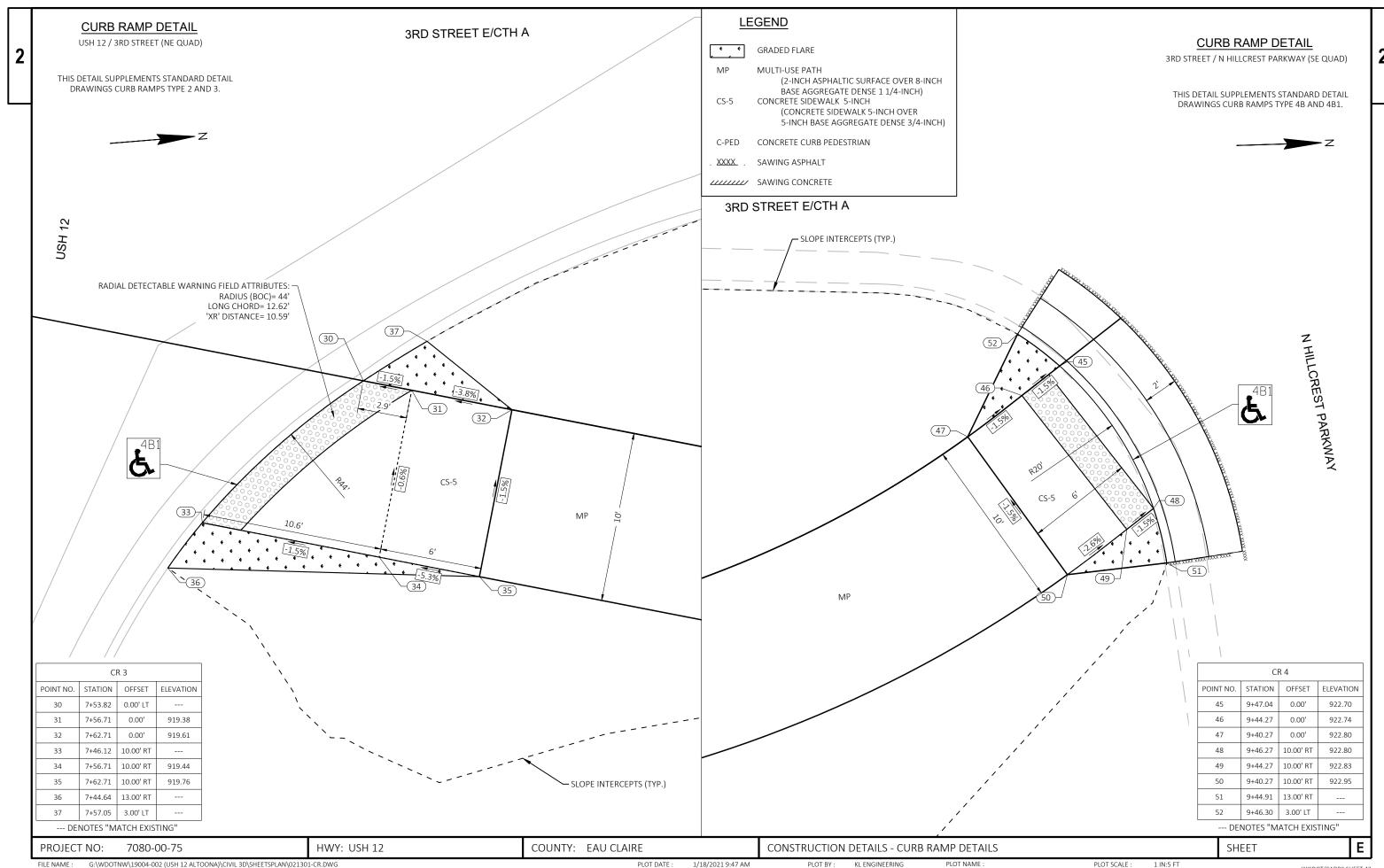


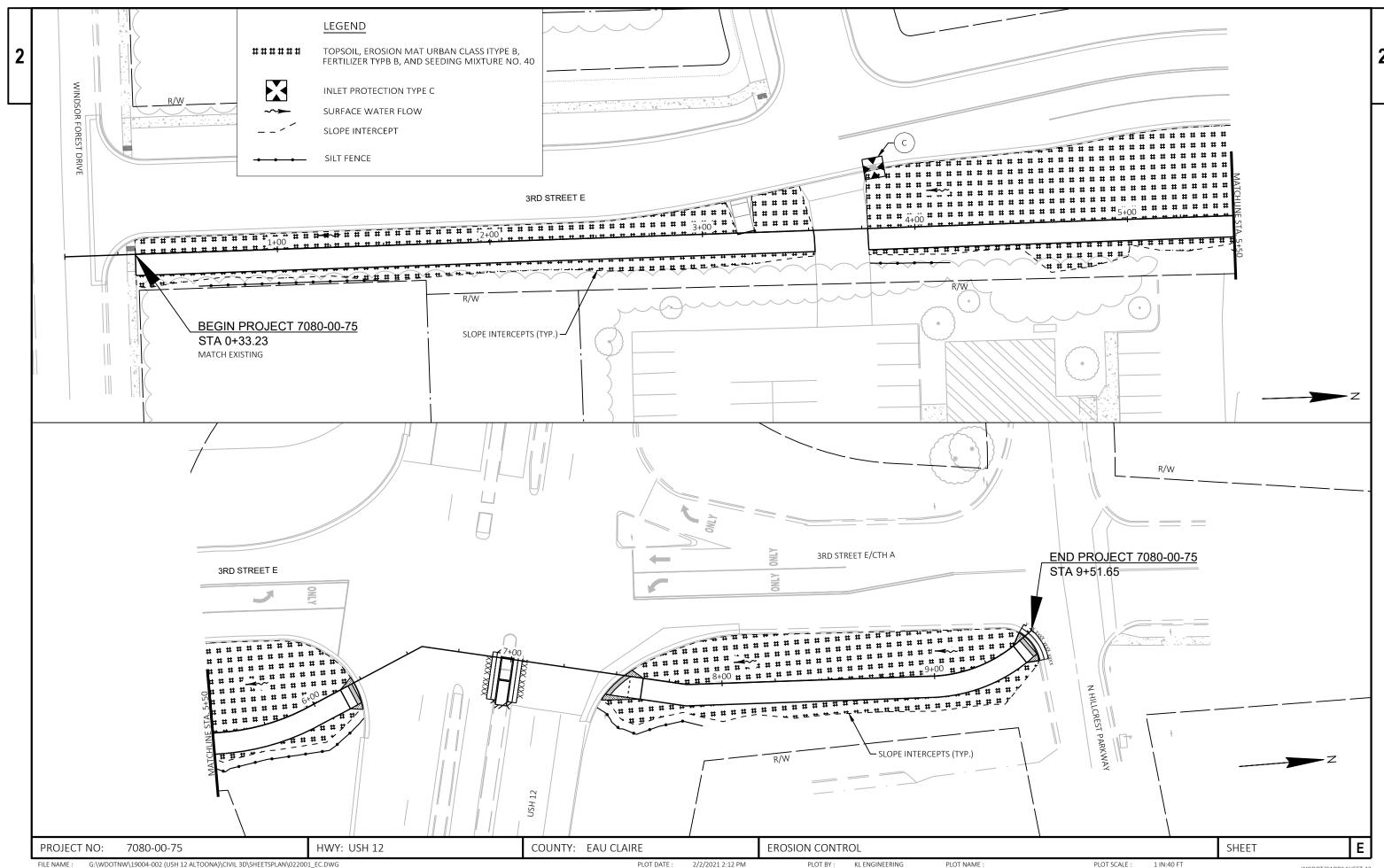


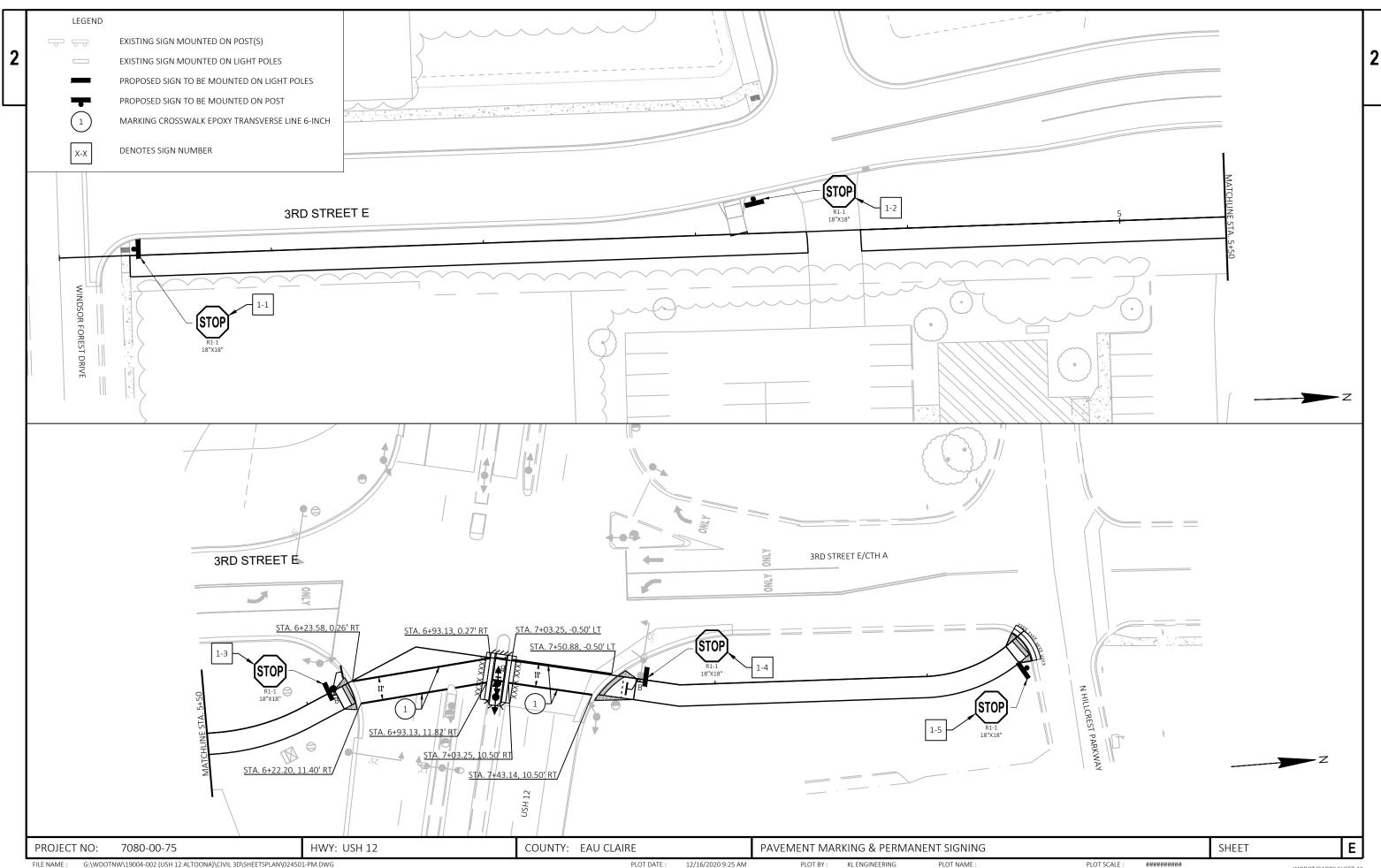


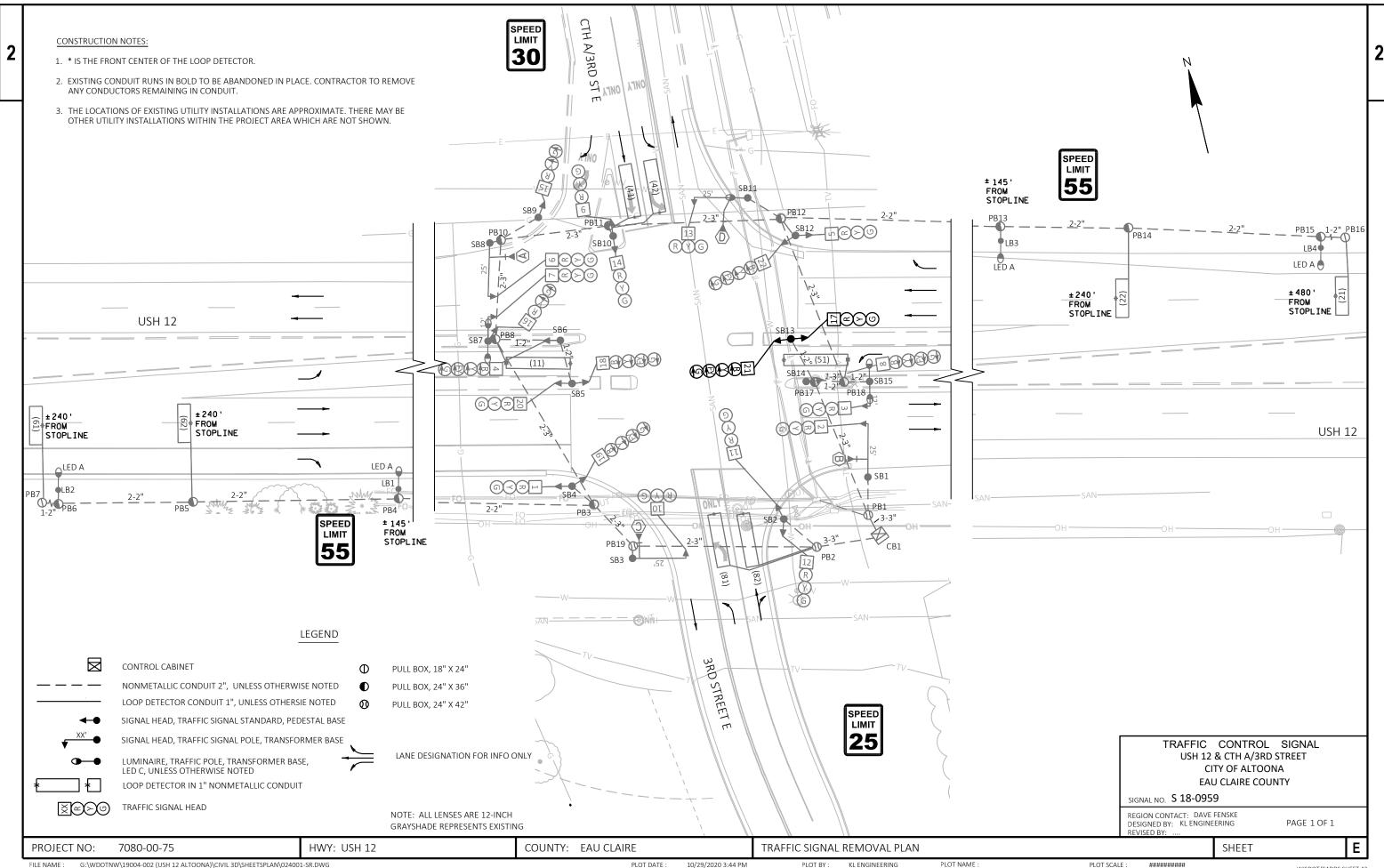


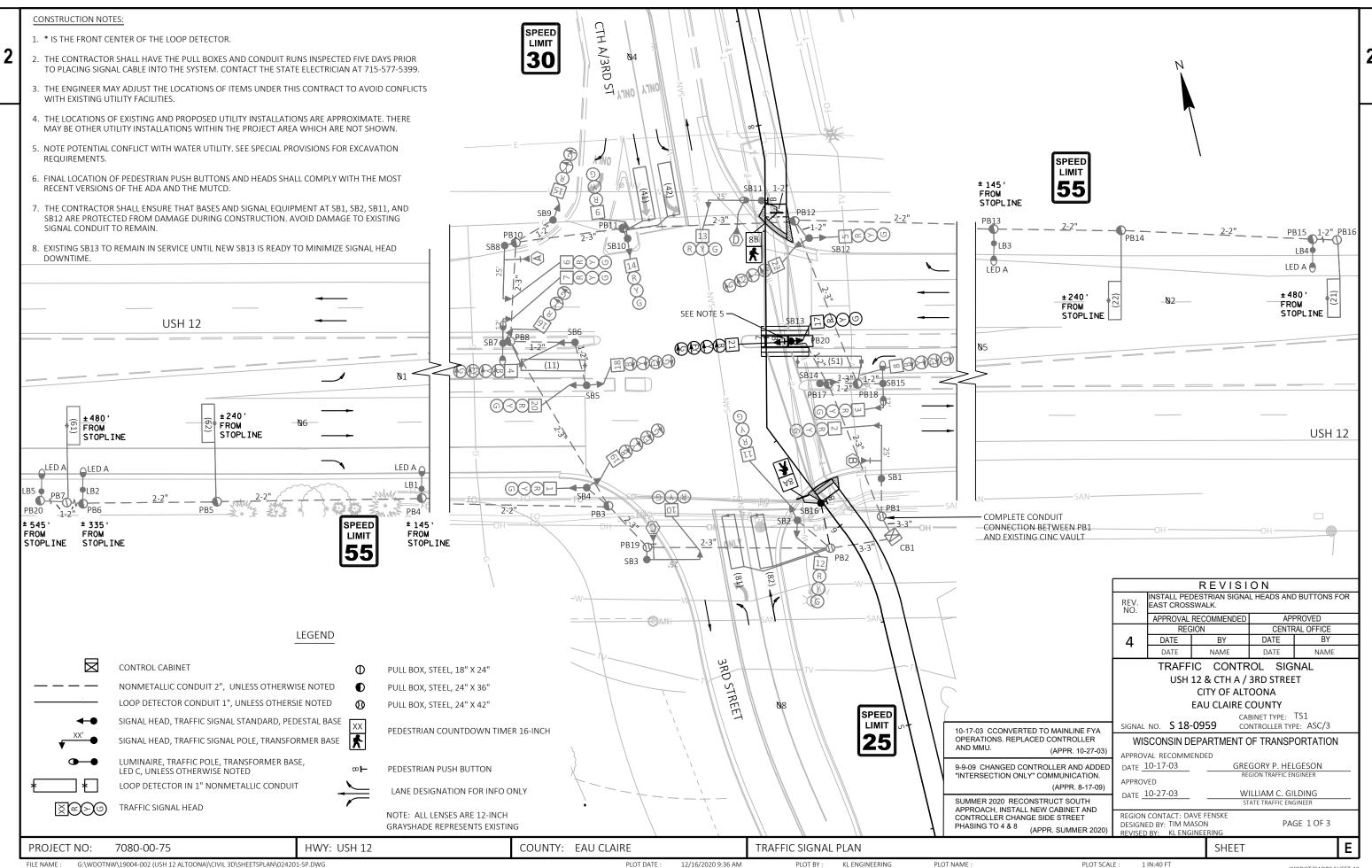
WISDOT/CADDS SHEET 42



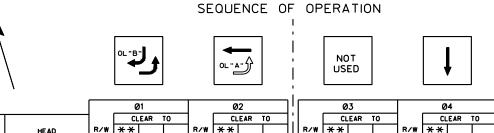








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\*\* CLEARANCE TO A PHASE IN CONFLICT WITH THIS PHASE ON (SEE CHART 1 BELOW)

BARRIER

WHEN CALLED, TIMED STEADY WALK, FOLLOWED BY FLASHING DON'T WALK WITH PED COUNTDOWN TIMER ACTIVATED, THEN STEADY DON'T WALK

		AMPLIFIER	DETEC	TOR OPE	RATION			DETECTOR			LOOP	NUMBER
LOOP NUMBER	DETECTOR NUMBER	CHANNEL	CALLS AND EXTENDS	CALLS ONLY	EXTENDS ONLY	PHASE CALLED	PHASE EXTENDED	DISCONNECT	CALLING DELAY	STRETCH	SIZE (FEET)	OF TURNS
11	1	1	х			1	1				6 X 30	4
21	2	2	x			2	2				6 X 20	4
22	2	2	x			2	2				6 X 20	3
41	4	3	x			4	4				6 X 25	3
42	4	4	x			4	4		(X=2")		6 X 25	3
51	5	5	x			5	5				6 X 30	4
61	6	6	x			6	6				6 X 20	4
62	6	6	x			6	6				6 X 20	3
81	8	7	x			8	8		(X=2")		6 X 25	3
82	8	8	x			8	8		(X=5")		6 X 25	3

CON	rroli	LER	LOGIC	

PHASE NUMBER	PHASE LOCKING	DUAL ENTRY W / Ø	PHASE RECALL	PHASE ACTIVE
1		6		×
2	×	6	MIN.	×
4		8		×
5		2		×
6	×	2	MIN.	×
8		4	·	×

### SPECIAL OVERLAPS

	PROTECTED	PERMISSIVE
0.L. "A" =	1	2
0.L. "F" =		
0.L. "C" =	5	6
0.L. "H" =		

### OVERLAPS

0.L. "A" 0.L. "B"	
O.L. "C"	=
O.L. "D"	=

### TYPE OF LIGHTING BY OTHER AGENCY IN TRAFFIC SIGNAL CABINET IN SEPARATE CONTROL CABINET

TYPE OF INTERCONNECT/COMMUNICATION						
NONE						
CLOSED LOOP						
TWISTED PAIR						
FIBER OPTIC*	х					
FIBER OPTIC (ETHERNET)						
RADIO						
CELL MODEM						

TYPE OF PRE-EMPT	
NONE	
RAILROAD	
EMERGENCY VEHICLE	×
GTT	X
TOMAR	
HARDWIRE	
OTHER	
LIFT BRIDGE	
QUEUE DETECTOR	

### EMERGENCY VEHICLE PREEMPTION SEQUENCE

EMERGENCY VEHICLE PREEMPTOR	A	В	С	D
PREEMPTION CHANNEL	1	2	3	4
MOVEMENT		$\uparrow \uparrow$	-	
DIRECTION	₩B	EB	SB	NB
PHASES	2+5	1+6	4+0L"B"	8

FULL CLEARANCE AND MINIMUM GREEN INTERVALS SHALL ALWAYS BE PROVIDED.

### CHART 1

PHASE ON	NONCONFLICTING PHASE ALLOWED TO TIME CONCURRENTLY	PHASES IN CONFLICT WITH PHASE ON
1	5 OR 6	2,4,8
2	5 OR 6	1,4,8
4	8	1,2,5,6
5	1 OR 2	4,6,8
6	1 OR 2	4,5,8
8	4	1256

### GENERAL NOTES:

- 1. ANY ACTUATED PHASE FOR WHICH THERE IS NO CALL SHALL BE SKIPPED.
- 2. WHEN ONE PHASE IS ON ALONE, ANY NON-CONFLICTING PHASE MAY START TIMING CONCURRENTLY WITHOUT A CLEARANCE INTERVAL. (SEE CHART 1 AT LEFT).
- 3. WHEN PHASES 4 AND 8 ARE TIMING, THEY SHALL TERMINATE TOGETHER DUE TO PERMISSIVE LEFT TURN CONFLICTS.

TRAFFIC CONTROL SIGNAL USH 12 & CTH A / 3RD STREET CITY OF ALTOONA **EAU CLAIRE COUNTY** 

SIGNAL NO. S 18-0959

REGION CONTACT: DAVE FENSKE DESIGNED BY: TIM MASON REVISED BY: KL ENGINEERING

PAGE 2 OF 3

HWY: USH 12

COUNTY: EAU CLAIRE

E

TRAFFIC SIGNAL PLAN

PLOT NAME :

SHEET

Ε

7080-00-75

PROJECT NO:

PROJECT NO: 7080-00-75 INTERSECTION: USH 12 & CTH A/3RD STREET

SIGNAL WIRE BLK-BLACK RED-RED GRN-GREEN COLOR CODING WHT-WHITE BLU-BLUE ORG-ORANGE

				SIGNAL INDICATION WIRE COLOR								
CB1 TO	AWG 14 # OF COND.	HEAD NO.	RED	YELLOW	GREEN	<red></red>	<yellow></yellow>	<green></green>	<flashing> <yellow></yellow></flashing>	DWALK	WALK	PED BUTTO
SB1	7	2	RED	ORG	GRN							
		_										
SB2	12	11	RED/BLK	ORG/BLK	GRN/BLK							
		12	RED	ORG	GRN							
SB3	7	10	RED	ORG	GRN							
SB4	12	1	RED	ORG	GRN							
		19				RED/BLK	ORG/BLK	GRN/BLK	BLK/WHT			
SB5	12	18				RED/BLK	ORG/BLK	GRN/BLK	BLK/WHT			
		20	RED	ORG	GRN							
SB6	7	4				RED	ORG	GRN	BLK			
SB7	12	7	RED	ORG	GRN							
		16	RED/BLK				ORG/BLK	GRN/BLK				
SB8	7	6	RED	ORG	GRN							
	<del>  _</del>											
SB9	7	15	RED				ORG	GRN				
CD40	42	0	DED /DL K	000/014	CDAL/DLI/							
SB10	12	9	RED/BLK	ORG/BLK	GRN/BLK							
		14	RED	ORG	GRN							
SB11	12	13	RED	ORG	GRN							
2011	12	8B	KED	ONG	GKIN					BLK	BLU	
		BUTTON								DLK	BLO	
		BOTTON										
SB12	12	5	RED/BLK	ORG/BLK	GRN/BLK							
3012	12	22	INEB/ BER	ONG/ BER	GITTY BEIT	RED	ORG	GRN	BLK/WHT			
						1125	5.1.5	5	22.9 *****			
SB13*	9	17	RED/BLK	ORG/BLK	GRN/BLK							
		21	,	5, 5	J, 22	RED	ORG	GRN	BLK/WHT			
		BUTTON							,			
SB14	7	8				RED	ORG	GRN	BLK			
SB15	7	3	RED	ORG	GRN							
SB16*	5	8A								BLK	BLU	
		BUTTON										

NOTE: ALL SIGNAL INDICATION WIRE PROVIDED FOR INFORMATION ONLY UNLESS NOTED WITH "\*."

EQUIPMENT GROUNDING							
CONDUCTORS 1	CONDUCTORS 10 AWG GRN XLP						
FROM TO							
CB1	SB16						
SB16	SB2						
SB13	SB14						
SB13	SB15						

LOOP DETECTOR LEAD IN CABLE					
FROM	TO				
CB1	SB11				
CB1	SB13				
CB1	SB16				

LIGHTING UF	2-12 AWG W/					
GRO	UND					
FROM	TO					
CB1	SB3					
SB3	LB1					
LB1	LB2					
SB3	SB7					
LB2	LB5					
SB3 LB1 LB1 LB2 SB3 SB7 LB2 LB5 CB1 SB15						
CB1	SB15					
SB15	SB11					
SB11	LB3					
NOTE: FOR INFORMA	ATION ONLY					

NOTE: FOR INFORMA	ATION ONLY
NOTE: FOR INFORM	ATION ONLY

EMERGENCY VEHI	CLE PREEMPTION
FROM	то
CB1	SB8 (HEAD A)
CB1	SB1 (HEAD B)
CB1	SB3 (HEAD C)
CB1	SB11 (HEAD D)
NOTE: FOR INFORMA	ATION ONLY

TRAFFIC CONTROL SIGNAL USH 12 & CTH A / 3RD STREET CITY OF ALTOONA EAU CLAIRE COUNTY

SIGNAL NO. S 18-0959

REGION CONTACT: DAVE FENSKE DESIGNED BY: TIM MASON REVISED BY: KL ENGINEERING

PAGE 3 OF 3

PROJECT NO: 7080-00-75 HWY: USH 12

COUNTY: EAU CLAIRE

TRAFFIC SIGNAL PLAN

G:\WDOTNW\19004-002 (USH 12 ALTOONA)\CIVIL 3D\SHEETSPLAN\024201-SP.DWG FILE NAME :

10/29/2020 4:35 PM

PLOT BY: KL ENGINEERING

PLOT SCALE : ##########

SHEET

<sup>\*</sup>USE THE WHITE CONDUCTOR IN THE CABLE ASSEMBLY AS THE GROUNDED CONDUCTOR FOR ALL TRAFFIC SIGNAL INDICATIONS

<sup>\*</sup>ENSURE THE GROUNDED CONDUCTOR IN THE FEEDER CABLE AND THE POLE CABLES ARE BOTH 12" LONGER THAN THE UNGROUNDED

<sup>\*</sup>AT THE SIGNAL BASES, CONNECT ONE TERMINAL FROM THE PEDESTRIAN PUSH BUTTONS TO THE COLOR INDICATED IN THE CHART. CONNECT THE OTHER TERMINAL TO THE GROUNDED CONDUCTOR
\*"OTHER" COLUMN MAY INCLUDE SHADOW BOX SIGN

2

2

WISDOT/CADDS SHEET 42

### GENERAL NOTES FOR TRAFFIC CONTROL

- 1) THE EXACT NUMBER, LOCATION AND SPACING OF SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- 2) ALL SIGNS ARE 48" X 48" UNLESS NOTED.
- 3) "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- 4) ALL TYPE II BARRICADES SHALL BE 4' WIDE, UNLESS OTHERWISE NOTED, AND EQUIPPED WITH TWO TYPE "A" (LOW INTENSITY FLASHING) LIGHTS.
- 5) WHEN A SEGMENT OF THE PROJECT IS NOT SHOWN ON THE STAGING PLANS, USE THE SAME TRAFFIC CONTROL AS THE PREVIOUS STAGE FOR THAT SEGMENT UNLESS OTHERWISE NOTED OR DIRECTED BY ENGINEER.
- 6) EXISTING AND ADVANCE WARNING TRAFFIC SIGNS MAY REQUIRE RELOCATION DURING CONSTRUCTION STAGING, INCIDENTAL TO THE CONTRACT.
- 7) MAINTAIN LOCAL ACCESS AT ALL TIMES UNLESS OTHERWISE NOTED IN THE TRAFFIC CONTROL PLANS.

### TRAFFIC CONTROL LEGEND

- TRAFFIC CONTROL BARRICADE TYPE II (WITH ATTACHED SIGN) WITH TRAFFIC CONTROL WARNING LIGHTS TYPE A
- TRAFFIC CONTROL BARRICADE TYPE III (WITH ATTACHED SIGN) WITH TRAFFIC CONTROL WARNING LIGHTS TYPE A
- TRAFFIC CONTROL SIGNS (ON TEMPORARY SUPPORT)
- TRAFFIC CONTROL DRUMS



WORK AREA

DIRECTION OF TRAFFIC

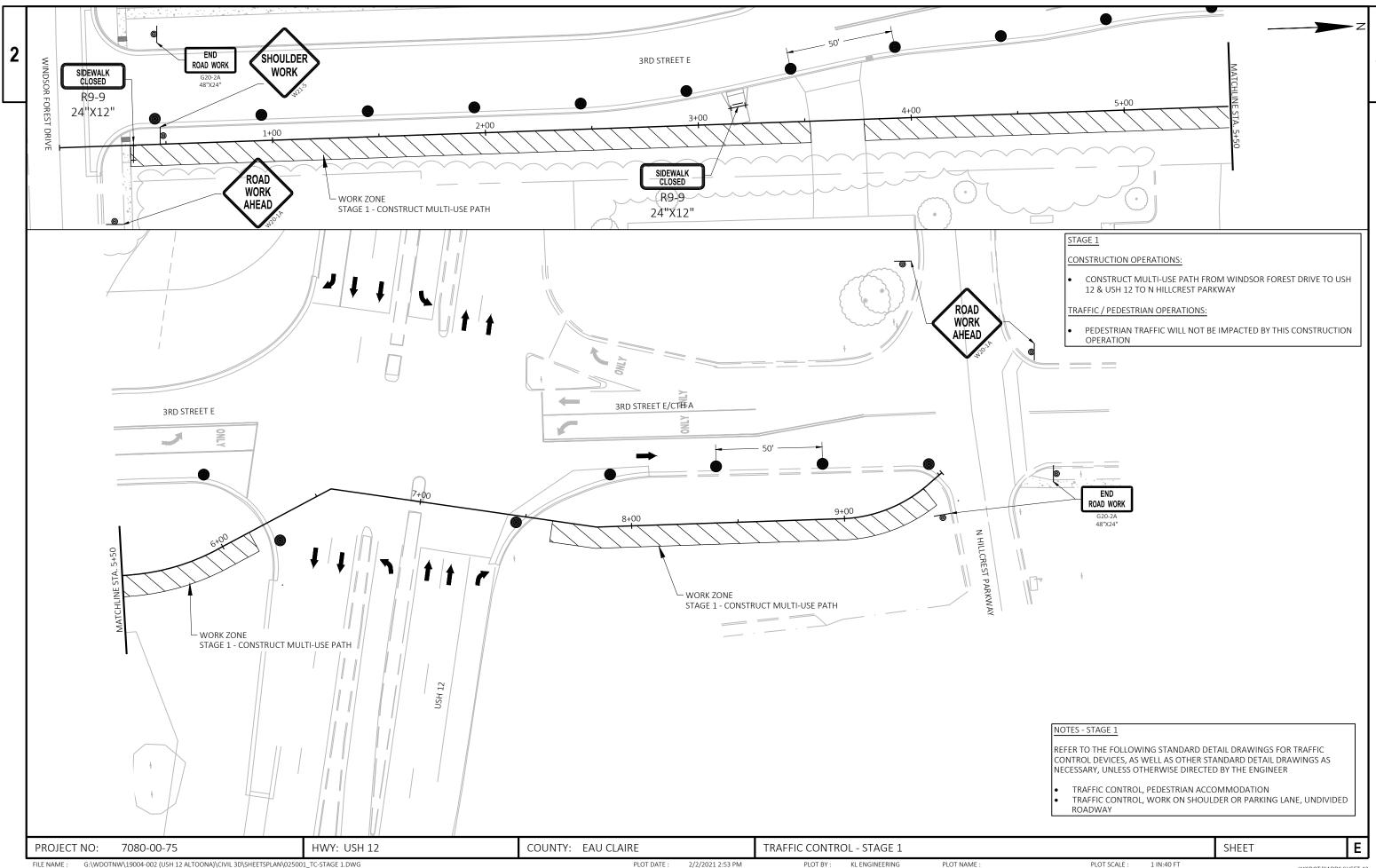
PROJECT NO: 7080-00-75 HWY: USH 12 COUNTY: EAU CLAIRE TRAFFIC CONTROL - NOTES & LEGEND SHEET **E** 

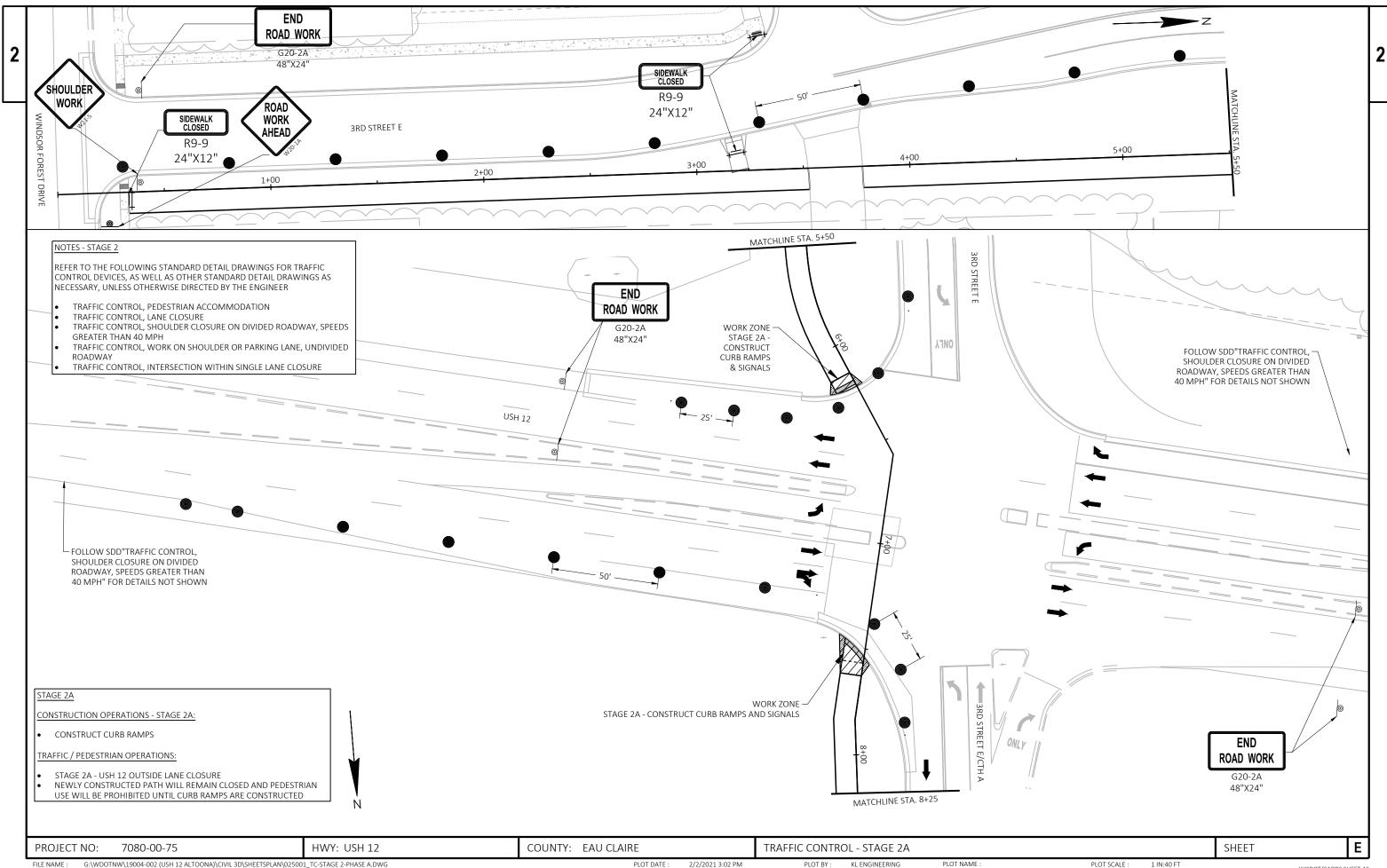
PLOT BY: KL ENGINEERING

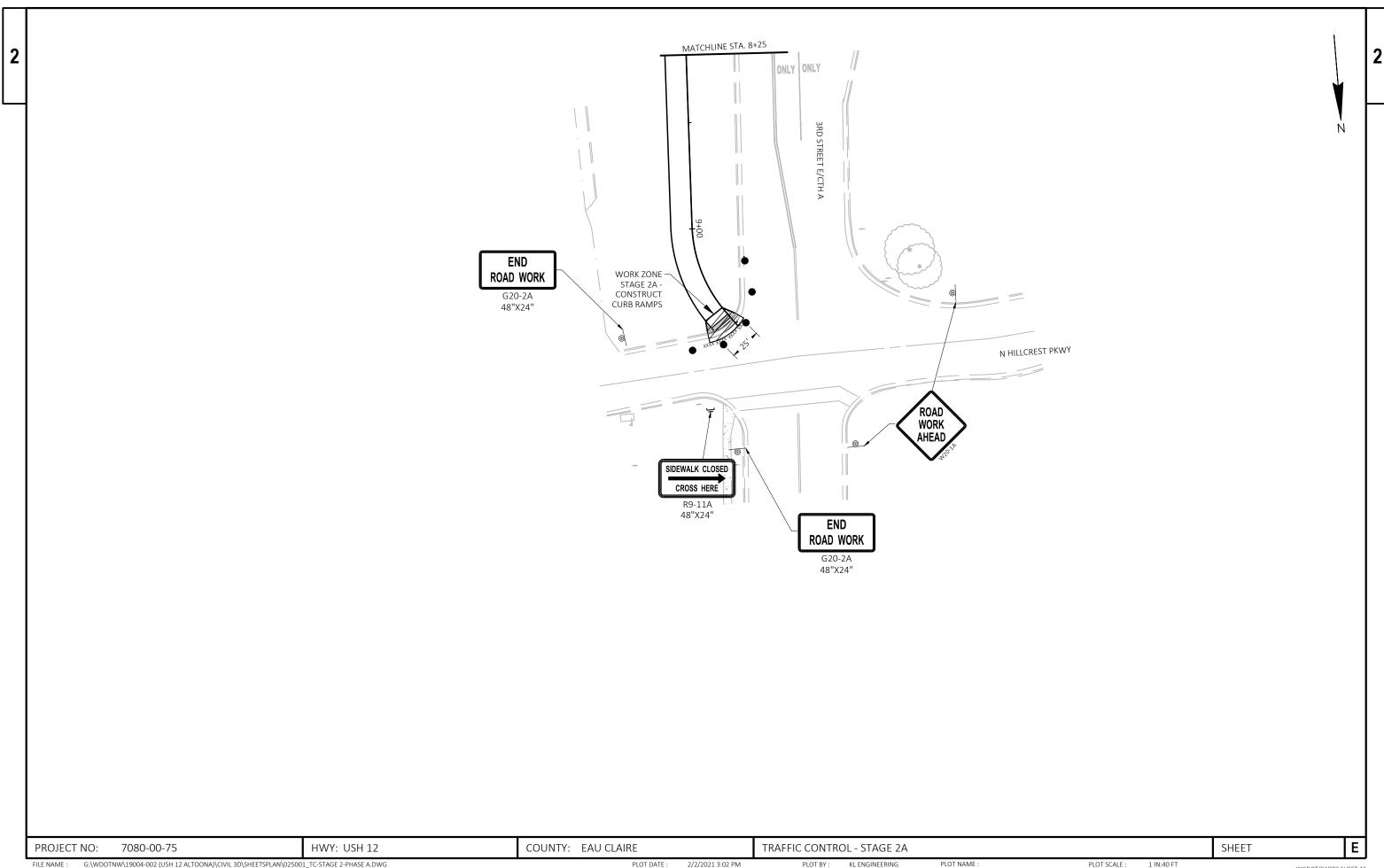
PLOT NAME :

PLOT SCALE: 1:1

PLOT DATE: 2/2/2021 3:19 PM







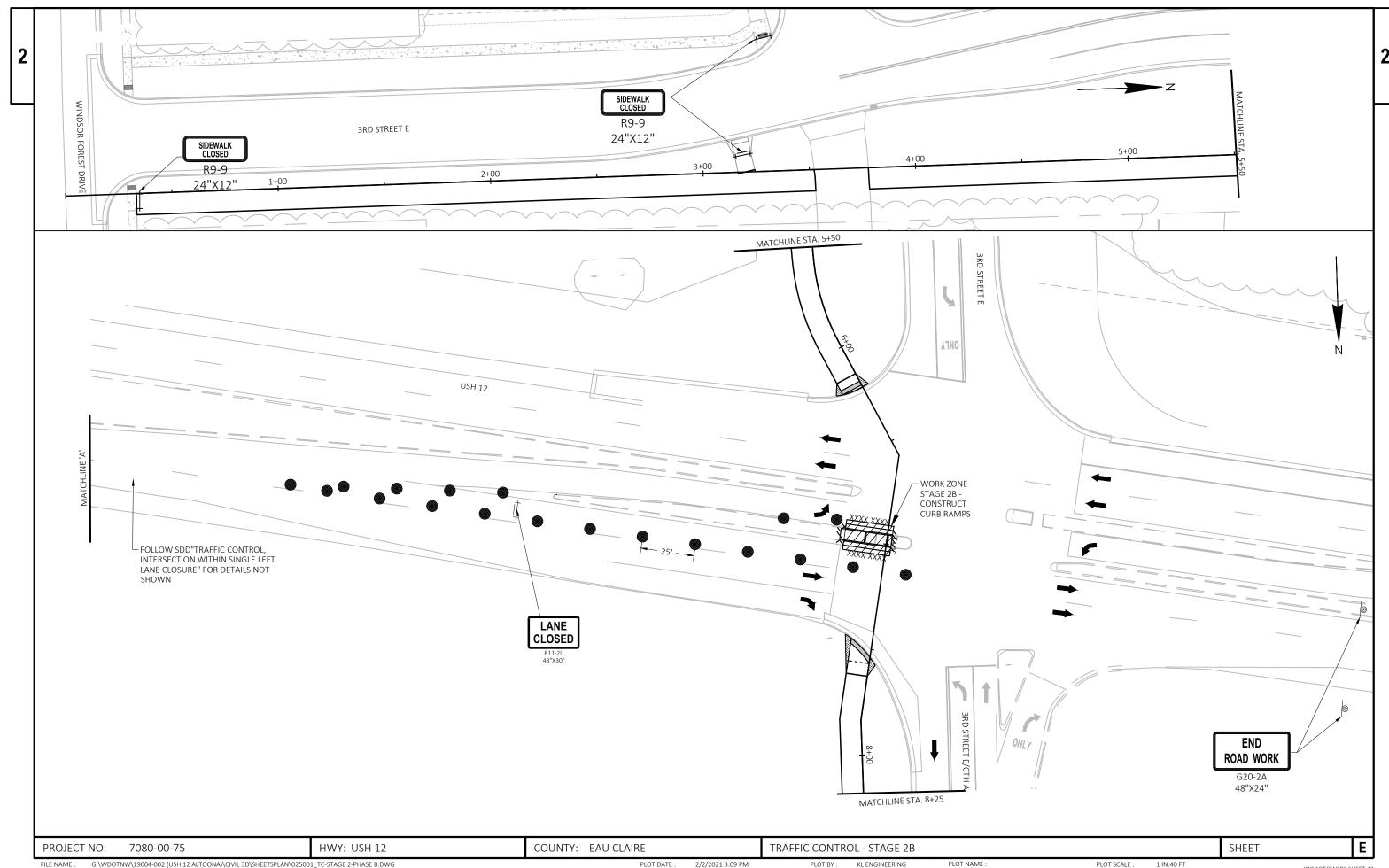
G:\WDOTNW\19004-002 (USH 12 ALTOONA)\CIVIL 3D\SHEETSPLAN\025001\_TC-STAGE 2-PHASE A.DWG LAYOUT NAME - 025001\_tc\_stage 2.2

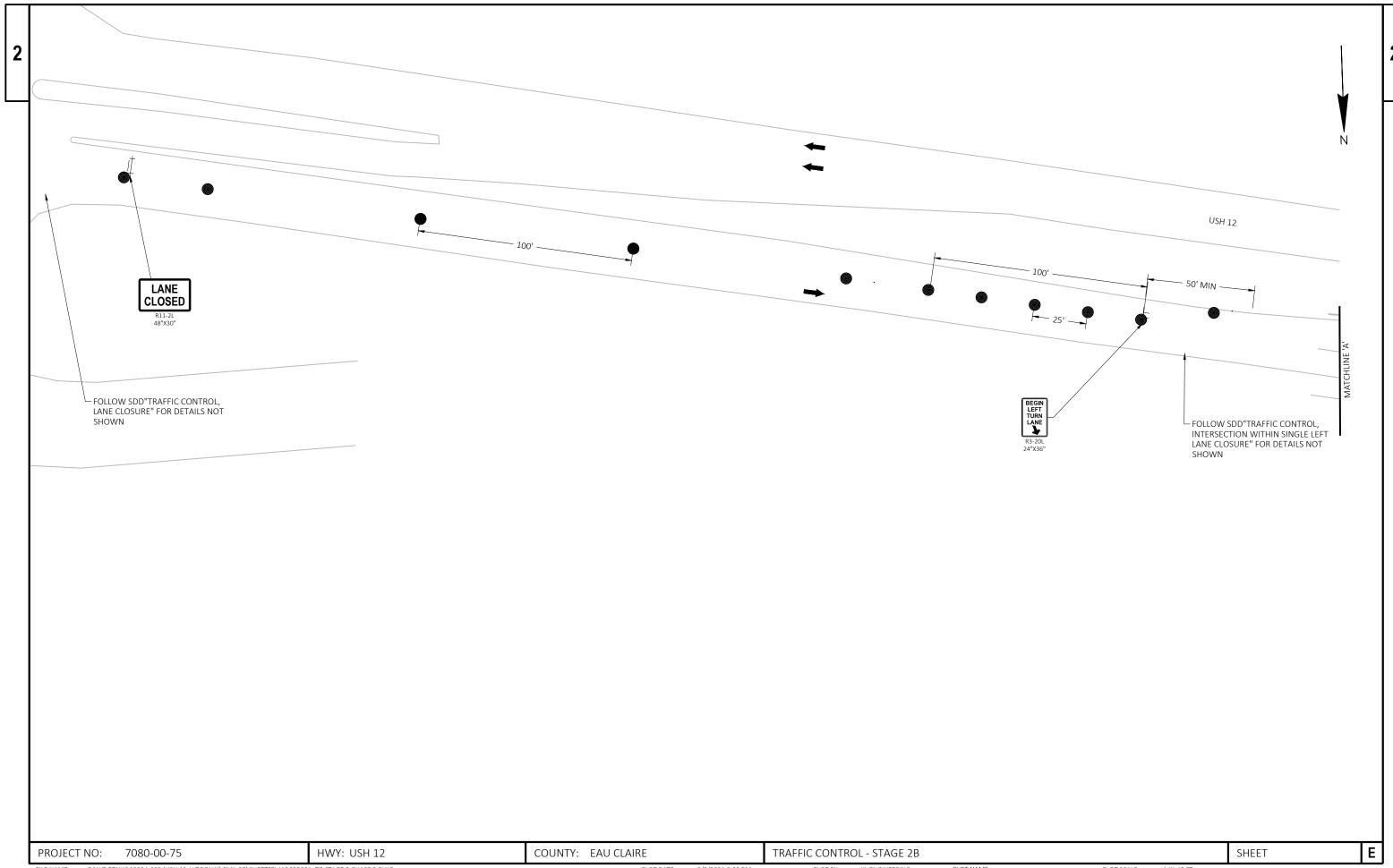
PLOT DATE : 2/2/2021 3:02 PM

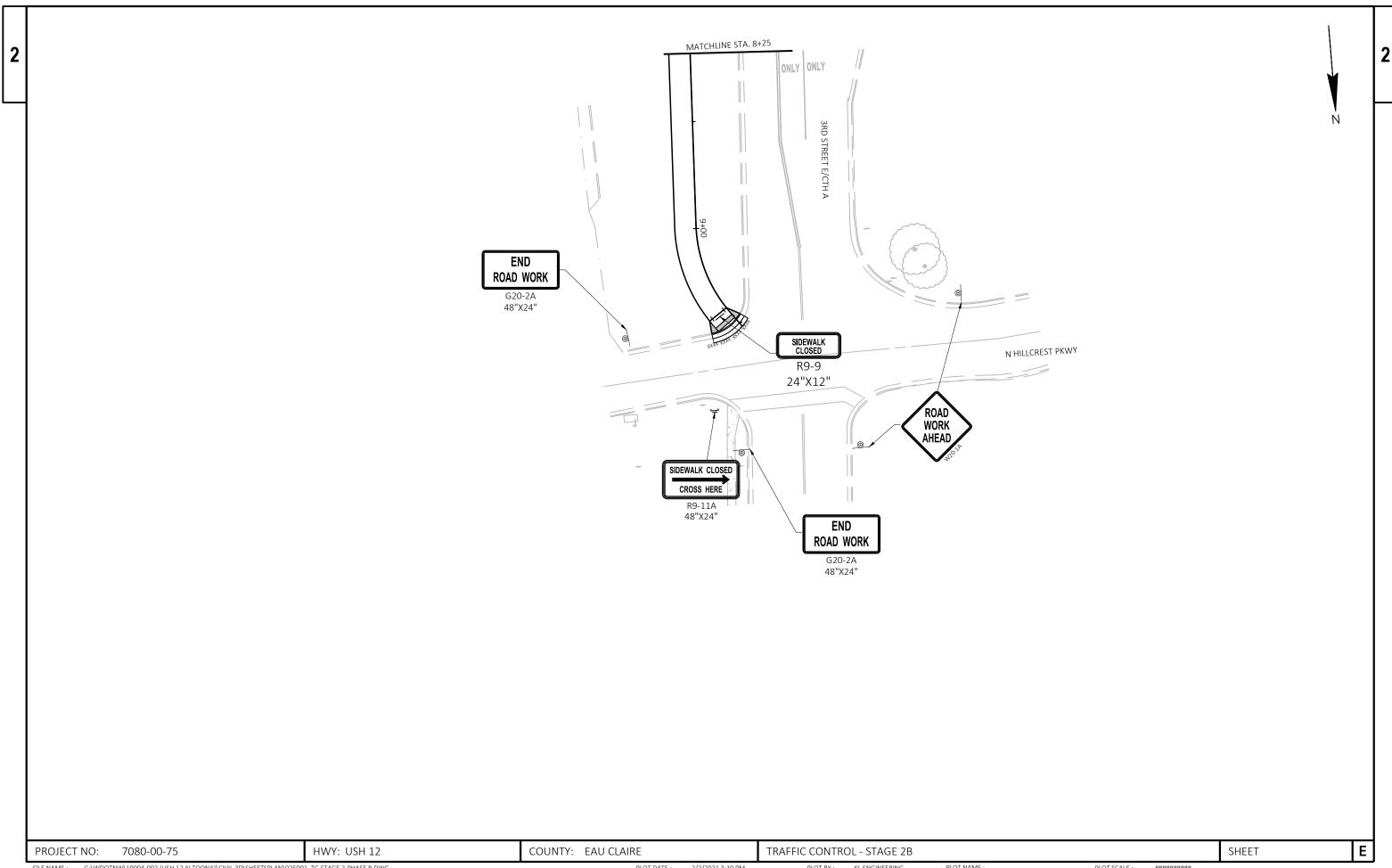
PLOT BY: KL ENGINEERING

PLOT NAME :

PLOT SCALE :







G:\WDOTNW\19004-002 (USH 12 ALTOONA)\CIVIL 3D\SHEETSPLAN\025001\_TC-STAGE 2-PHASE B.DWG LAYOUT NAME - 025001\_tc - stage 2 phase b (3)

PLOT DATE: 2/2/2021 3:10 PM

PLOT BY: KL ENGINEERING

PLOT NAME :

PLOT SCALE :

					7080-00-75
Line	Item	Item Description	Unit	Total	Qty
0002	204.0150	Removing Curb & Gutter	LF	70.000	70.000
0004	204.0155	Removing Concrete Sidewalk	SY	14.000	14.000
0004	204.0195	Removing Concrete Bases	EACH	1.000	1.000
0008	205.0100	Excavation Common	CY	400.000	400.000
0010	213.0100	Finishing Roadway (project) 001. 7080-00-75	EACH	1.000	1.000
0012	305.0110	Base Aggregate Dense 3/4-Inch	TON	20.000	20.000
0012	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	560.000	560.000
0014	465.0105	Asphaltic Surface	TON	100.000	100.000
0018	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	70.000	70.000
0010	601.0600	Concrete Curb Pedestrian	LF	10.000	10.000
0020	602.0410	Concrete Sidewalk 5-Inch	SF	400.000	400.000
0022	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	40.000	40.000
			SF		
0026	602.0605	Curb Ramp Detectable Warning Field Radial Yellow		26.000	26.000
0028	618.0100	Maintenance And Repair of Haul Roads (project) 001. 7080-00-75	EACH	1.000	1.000
0030	619.1000	Mobilization	EACH	1.000	1.000
0032	624.0100	Water	MGAL	8.000	8.000
0034	625.0100	Topsoil	SY	2,600.000	2,600.000
0036	628.1504	Silt Fence	LF	325.000	325.000
0038	628.1520	Silt Fence Maintenance	LF	1,000.000	1,000.000
0040	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0042	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0044	628.2008	Erosion Mat Urban Class I Type B	SY	2,600.000	2,600.000
0046	628.7015	Inlet Protection Type C	EACH	3.000	3.000
0048	629.0210	Fertilizer Type B	CWT	5.000	5.000
0050	630.0140	Seeding Mixture No. 40	LB	50.000	50.000
0052	630.0200	Seeding Temporary	LB	70.000	70.000
0054	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	3.000	3.000
0056	637.2210	Signs Type II Reflective H	SF	9.300	9.300
0058	642.5001	Field Office Type B	EACH	1.000	1.000
0060	643.0300	Traffic Control Drums	DAY	1,950.000	1,950.000
0062	643.0410	Traffic Control Barricades Type II	DAY	30.000	30.000
0064	643.0420	Traffic Control Barricades Type III	DAY	210.000	210.000
0066	643.0705	Traffic Control Warning Lights Type A	DAY	240.000	240.000
0068	643.0715	Traffic Control Warning Lights Type C	DAY	200.000	200.000
0070	643.0800	Traffic Control Arrow Boards	DAY	60.000	60.000
0070	643.0900	Traffic Control Signs	DAY	1,350.000	1,350.000
0072	643.5000	Traffic Control	EACH	1,330.000	1,330.000
0074					
	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	210.000	210.000
0078	650.4500	Construction Staking Subgrade	LF	756.000	756.000

# Estimate Of Quantities Page 2

					7080-00-75
Line	Item	Item Description	Unit	Total	Qty
0800	650.5000	Construction Staking Base	LF	756.000	756.000
0082	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	62.000	62.000
0084	650.8500	Construction Staking Electrical Installations (project) 001. 7080-00-75	LS	1.000	1.000
0086	650.9000	Construction Staking Curb Ramps	EACH	4.000	4.000
8800	650.9910	Construction Staking Supplemental Control (project) 001. 7080-00-75	LS	1.000	1.000
0090	650.9920	Construction Staking Slope Stakes	LF	756.000	756.000
0092	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	30.000	30.000
0094	652.0700.S	Install Conduit into Existing Item	EACH	2.000	2.000
0096	654.0101	Concrete Bases Type 1	EACH	1.000	1.000
0098	655.0230	Cable Traffic Signal 5-14 AWG	LF	140.000	140.000
0100	655.0250	Cable Traffic Signal 9-14 AWG	LF	168.000	168.000
0102	655.0515	Electrical Wire Traffic Signals 10 AWG	LF	575.000	575.000
0104	655.0700	Loop Detector Lead In Cable	LF	513.000	513.000
0106	657.0100	Pedestal Bases	EACH	2.000	2.000
0108	657.0425	Traffic Signal Standards Aluminum 15-FT	EACH	1.000	1.000
0110	657.0430	Traffic Signal Standards Aluminum 10-FT	EACH	1.000	1.000
0112	658.0173	Traffic Signal Face 3S 12-Inch	EACH	1.000	1.000
0114	658.0174	Traffic Signal Face 4S 12-Inch	EACH	1.000	1.000
0116	658.0416	Pedestrian Signal Face 16-Inch	EACH	2.000	2.000
0118	658.0500	Pedestrian Push Buttons	EACH	3.000	3.000
0120	671.0112	Conduit HDPE 1-Duct 2-Inch	LF	50.000	50.000
0122	690.0150	Sawing Asphalt	LF	90.000	90.000
0124	690.0250	Sawing Concrete	LF	35.000	35.000
0126	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0128	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0130	SPV.0060	Special 001. Concrete Bases Type 1 Special	EACH	1.000	1.000
0132	SPV.0105	Special 001. Remove Traffic Signals (USH 12 & 3RD ST)	LS	1.000	1.000

	REMOVING CURB & GUTTER	
		204.0
CATEGORY	STATION TO STATION	(LF
0010		
	3RD STREET E (SOUTH)	
	0+33 - 6+26	-
	USH 12	
	6+94 - 7+04	45
	3RD STREET E (NORTH)	
	7+49 - 9+52	16
	UNDISTRIBUTED	9
PROJECT 7080	UNDISTRIBUTED 0-00-75 TOTAL - CATEGORY 0010	

## REMOVING CONCRETE SIDEWALK

		204.015
CATEGORY	STATION TO STATION	(SY)
0010		
	3RD STREET E (SOUTH)	
	0+33 - 6+26	-
	USH 12	
	6+94 - 7+04	14
	3RD STREET E (NORTH)	
	7+49 - 9+52	-
PROJECT 708	0-00-75 TOTAL - CATEGORY 0010	14

### BASE AGGREGATE DENSE

305.0110 305.0120 624.0100

CATEGORY	STATION TO STATION	3/4 - INCH (TON)	1 1/4 - INCH (TON)	WATER (MGAL)
0010				
	3RD STREET E (SOUTH)			
	0+33 - 6+26	3	403	4
	USH 12			
	6+94 - 7+04	5	9	1
	3RD STREET E (NORTH)			
	7+49 - 9+52	9	131	1
	UNDISTRIBUTED	4	17	1
DDO IFOT 700	0.00 TE TOTAL CATEGORY 0040		F00	
PROJECT 7080	0-00-75 TOTAL - CATEGORY 0010	20	560	8

### ASPHALTIC SURFACE

		465.010
CATEGORY	STATION TO STATION	(TON)
0010		
	3RD STREET E (SOUTH)	
	0+33 - 6+26	73
	USH 12	
	6+94 - 7+04	1
	3RD STREET E (NORTH)	
	7+49 - 9+52	23
	UNDISTRIBUTED	3
PROJECT 708	0-00-75 TOTAL - CATEGORY 0010	100

								EARTHW	/ORK									
					205	5.0100	Salvaged/			Reduced	Reduced	Expanded	Unexpanded	Expanded	Mass	Waste	208.0100	Comment:
					Common E	excavation (1)	Unusable			Marsh	EBS	Marsh	Fill	Fill	Ordinate		Borrow	
						EBS	Pavement	Available	Marsh	in Fill	in Fill	Backfill			+/-			
					Cut	Excavation	Material	Material	Excavation					(5)	(6)			
CATEGORY	STAGE	DIVISION	LOCATION	STATION TO STATION	(2)		(3)	(4)		Factor	Factor	Factor		Factor				
					(CY)	(CY)			(CY)	0.60	0.80	1.50		1.25			(CY)	
0010	1	1	PATH	0+33 - 6+26	298	0	0	298	0	0	0	0	115	144	153	153	0	
				7+49 - 9+52	102	0	0	102	0	0	0	0	30	37	65	65	0	
	TOTAL				400													

1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS

HWY: USH 12

- 2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT
- 3) SALVAGED/UNUSABLE PAVEMENT MATERIAL
- 4) AVAILABLE MATERIAL = CUT SALVAGED/UNUSUABLE PAVEMENT MATERIAL
- 5) EXPANDED FILL/SELECT FILL FACTOR = 1.25
- 6) THE MASS ORDINATE + OR QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

### CONCRETE CURB & GUTTER

601 0411

601 0600

10

		001.0411	001.0000
			CONCRETE
		30-INCH	CURB
		TYPE D	PEDESTRIAN
CATEGORY	STATION TO STATION	(LF)	(LF)
0010			
	3RD STREET E (SOUTH)		
	0+33 - 6+26	-	-
	USH 12		
	6+94 - 7+04	45	10
	3RD STREET E (NORTH)		
	7+49 - 9+52	17	-
	UNDISTRIBUTED	8	-

	MARKING	
		646.7420
		CROSSWALK
		EPOXY
		TRANSVERSE LINE
		6-INCH
CATEGORY	STATION TO STATION	(LF)
0010		
	USH 12 EB	122
	USH 12 WB	88
PROJECT 7080	0-00-75 TOTAL - CATEGORY 0010	210

### MOBILIZATIONS EROSION CONTROL

		628.1905	628.1910
			MOBILIZATIONS
		MOBILIZATIONS	EMERGENCY
		EROSION	EROSION
		CONTROL	CONTROL
CATEGORY		(EACH)	(EACH)
0010	PROJECT 7080-00-75	3	2
ROJECT 7080-00	-75 TOTAL - CATEGORY 0010	3	2

MISCELLANEOUS QUANTITIES

SHEET

FILE NAME : G:\WDOTNW\19004-002 (USH 12 ALTOONA)\CIVIL 3D\SHEETSPLAN\030201\_MQ.DWG LAYOUT NAME - 01

7080-00-75

PROJECT NO:

COUNTY: EAU CLAIRE

PLOT DATE : 2/2/2021 4:34 PM

PLOT BY: KL ENGINEERING

PLOT NAME :

PLOT SCALE : Custom

PROJECT 7080-00-75 TOTAL - CATEGORY 0010

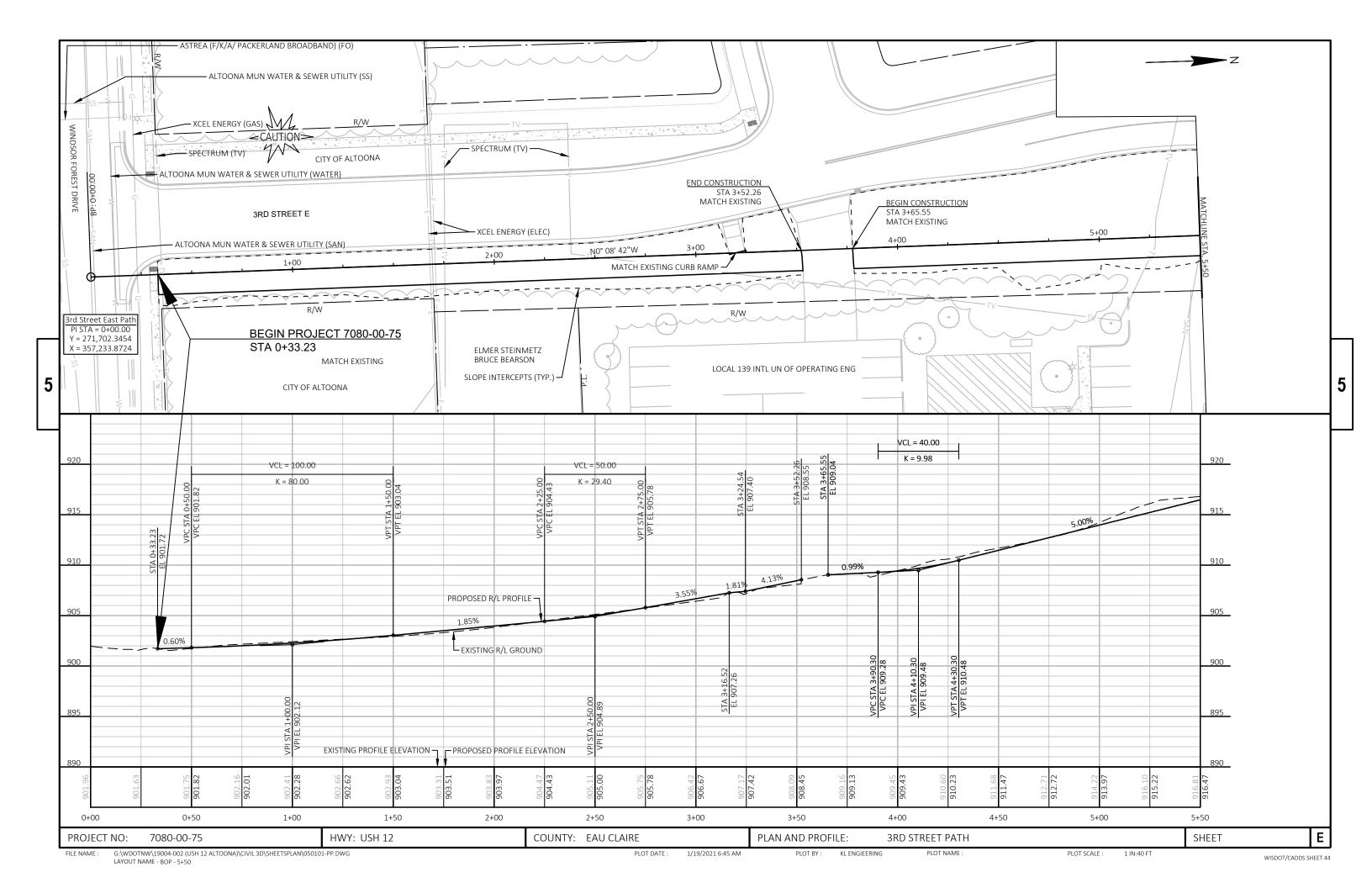
WISDOT/CADDS SHEET 42

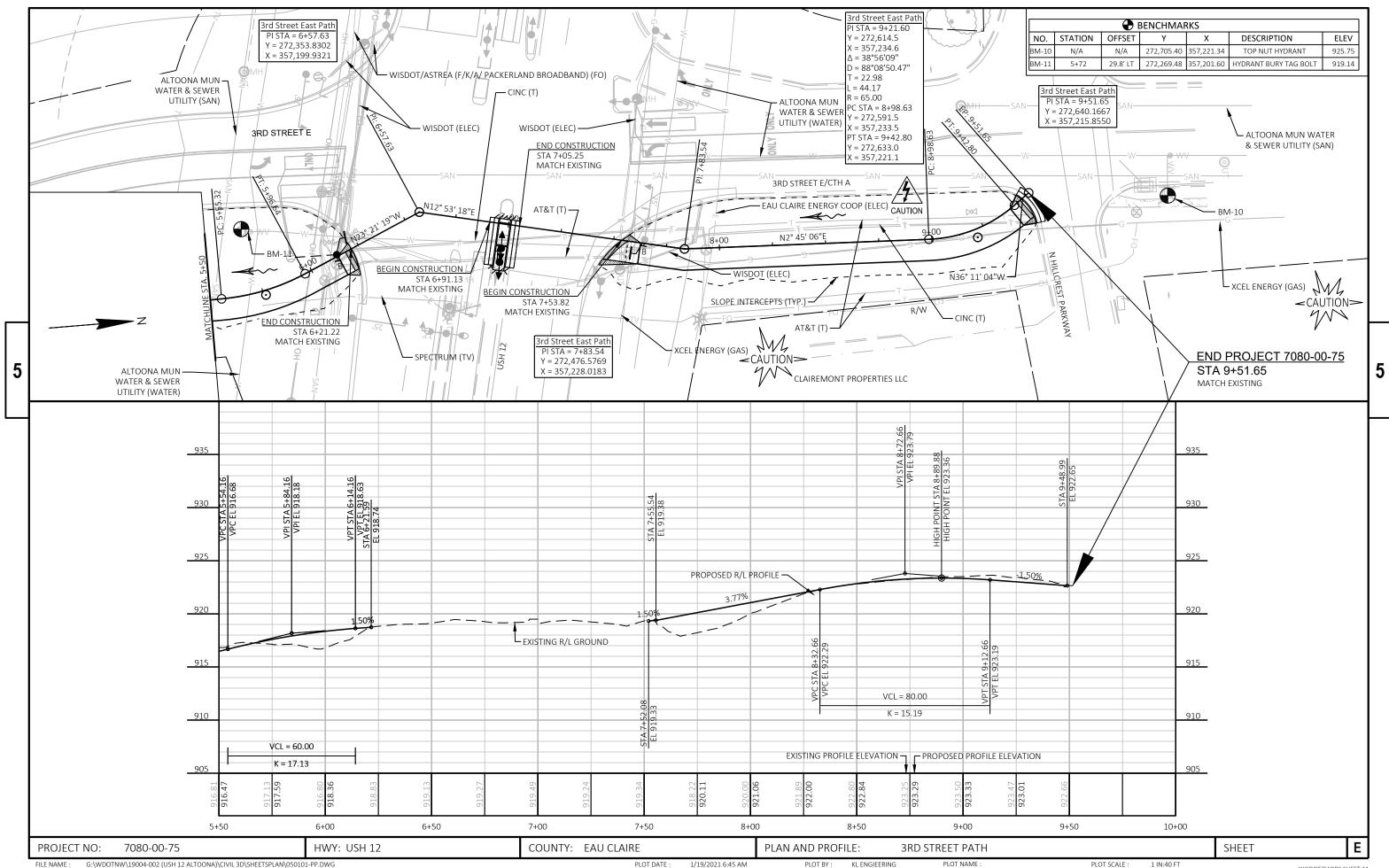
70

	CONCRET	TE SIDEWALK														
		602.0410	602.0505	602.0605			LANDSCAPING	GITEMS					SAWING			
							625.0100	629.0210	630.0140	630.0200			oo	690.0150	600.0250	
			CURB RAMP DETECTABLE	CURB RAMP DETECTABLE				FERTILIZER	SEEDING	SEEDING				690.0150	690.0250	
			WARNING FIELD		CATE	COODY OTATION TO OTATION	TOPSOIL	TYPE B	MIXTURE NO. 40			OATEOODY OTA	TION TO STATION	ASPHALT		E
CATEGO	RY STATION TO STATION	5-INCH (SF)	YELLOW (SF)	RADIAL YELLOW (SF)		EGORY STATION TO STATION 010	(SY)	(CWT)	(LB)	(LB)	-   -	0010	ATION TO STATION	(LF)	(LF)	
0010						3RD STREET E (SOUTH)					_		STREET E (SOUTH)			
	3RD STREET E (SOUTH) 0+33 - 6+26	69	20			0+33 - 6+26	1,526	3	27	41		0+3	3 - 6+26	-	-	
						USH 12					_		112			
	USH 12 6+94 - 7+04	120	_			6+94 - 7+04	-	-	-	-		6+9	4 - 7+04	53	24	
		120	-	-		3RD STREET E (NORTH)					_		STREET E (NORTH)			
	3RD STREET E (NORTH) 7+49 - 9+52	20.4	20	26		7+49 - 9+52	540	1	10	15		7+4	9 - 9+52	24	5	
	1140-0134	204	20	26		UNDISTRIBUTED	534	1	13	14		UNE	DISTRIBUTED	13	6	
	UNDISTRIBUTED	7	-	-	PROJE	ECT 7080-00-75 TOTAL - CATEGORY 0010	2,600	5	50	70	_   _	PO IECT 7080-00-7	'5 TOTAL - CATEGORY 001	0 90	35	
PROJECT	7080-00-75 TOTAL - CATEGORY 0010	400	40	26	PROSE	ST 7000-00-73 TOTAL - CATEGORT WITE	2,500	3	30	70		KOSECT 7000-00-7	STOTAL - GATEGORT WIT	0 30	33	
		EROSION	CONTROL							<b></b>	TRAFFIC CO		0:	040 5747	0.00.00	0.40.00.
		628.15	504 628.1	1520 628.2008	628.7015					643.0300	643.0410	643.0420	643.0705	643.0715	643.0800	643.0900
				EROSION M	1AT INLET						BARRICADES		WARNING LIGHTS			
		011 T FF	SILT F	ENCE URBAN CLA	SS I PROTECTIO	DN	CATEGORY	STATION TO STATIO		O. OF DRUMS DAYS (DAY)	TYPE II (DAY)	TYPE III (DAY)	TYPE A (DAY)	TYPE C (DAY)	BOARDS (DAY)	SIGNS (DAY)
CATE	EGORY STATION TO STATION	SILT FEI (LF)			TYPE C (EACH)		0010	CIATION TO STATIO	, L	AIO (DAI)	(5/11)	(5/11)	(5/11)	(2/11)	(5/11)	(3/11)
0	0010 3PD STREET E (SOUTH)					_	_	STAGE 1 STAGE 1		27 486		54	54			040
	3RD STREET E (SOUTH) 0+33 - 6+26	205	61	14 1,501	1	_		OIAGE I		486	-	54	54	-	-	243
							-	STAGE 2		40	40				24	0.10
	USH 12 6+94 - 7+04					<u> </u>		STAGE 2A		12 384	12	36	48	-	24	312
							-	STAGE 2B		12 674	12	72	84	158	24	513
	3RD STREET E (NORTH) 7+49 - 9+52	56	16	S7 550		_		UNDISTRIBUTED		405	6	48	54	42	12	282
	1140-0102	56	16	550	-						0	40			12	
	UNDISTURBED	65	21	19 550	2		PROJECT 7080-0	00-75 TOTAL - CATEO	SORY 0010	1,950	30	210	240	200	60	1,350
PROJI	ECT 7080-00-75 TOTAL - CATEGORY 0010	0 325	1,0	2,600	3		* FOR REFERENCE	EONLY								
											CONS	STRUCTION STAKI	NG			
			PERMANENT S	SIGNING								650.4500 65	60.5000 650.5500	650.9000 65	0.9920	
				637.2210	634.0612								CURB GUTTER			
				037.2210	034.0012								AND CURB	CURB S	LOPE	
					POSTS WOOD				CATEGORY ST	ATION TO STATION			BASE & GUTTER (LF) (LF)		TAKES (LF)	
			WIDTH HEIGH	SIGNS TYPE II HT REFLECTIVE H	4X6-INCH X 12-FT				0010	on to oranion		\ <del></del> : /	, (=, )	(= .0)	<u> </u>	
CATEG		N MESSAGE	(IN) (IN)	(SF)	(EACH)	NOTES				D STREET E (SOUTH	H)	500	500		500	
001		TOP SIGN TOP SIGN	18 18 18 18	1.86 1.86	1				0+3	33 - 6+26		562	562 -	1	562	
		TOP SIGN	18 18	1.86		PLACE ON SIGNAL POLE				H 12						
		TOP SIGN	18 18	1.86		PLACE ON SIGNAL POLE			6+9	94 - 7+04		-	- 45	1	-	
	1-5 R1-1 S <sup>-</sup>	TOP SIGN	18 18	1.86	1				3RI	D STREET E (NORT)	H)					
PROJEC	T 7080-00-75 TOTAL - CATEGORY 0010			9.30	3				7+4	19 - 9+52		193	193 17	2	193	
									PROJECT 7080-00-	75 TOTAL - CATEGO	PRY 0010	756	756 62	4	756	

		-	INAFFIC	SIGNAL C	JADLE /	AND WINE - ABC	OVE GROUND													
						6/	55.0230 *	655.0700	O *											
							LE TRAFFIC AL 5-14 AWG	LOOP DETE												
_		N FROM	I SIGNAL E	3ASE -	TO SIGN		LF	LF					TRA	FFIC SIGN	AL CABLE	AND WIRE -	- BELO	W GROUND		
	CATEGORY																			
	INTERSECT	iON	SB11			8B	15									655.0250		655.0515	655.0700	) *
			SB11	-		OB JTTON	15 	 6								BLE		CTRICAL WIRE		
			SB13	_		17	19							-		SIGNAL 9-14 AWG		10 AWG	LOOP DETEC	
			SB13	-	:	21	22					LOCATION	FROM	- TO	LF	J-14 AVVG	,	LF	LEAD IN CA	IDEL
			SB13			JTTON		6				CATEGORY 0010						<del>_</del> -		
			SB16	-		A8 ITTON	15					INTERSECTION								
			SB16	-	BO	JTTON		б						- SB11	-			-	258	
-	INTERSECT	ION TOTA	L				71	18						- SB13		168		-	168	
														- SB16 - SB2	69 <del></del>			85 88	69 	
	* ITEM ALSO	) LISTED EI	LEWHERE	Ē										- SB15				218		
				TDA	EEIC CI	CNAL CONDUIT							0.0.4.0	- SB14				84		
				IRA	FFIC SIC	GNAL CONDUIT							SB13	- SB15				100		
					6	352.0225	652.07	00.S 6	371.0112			INTERSECTION T			69	168			495	
				CON		IGID NONMETALL						INTERSECTION	IOIAL		69	100		575	495	
						HEDULE 40	INTO		1-DUCT			* ITEM ALSO LISTI	ED ELEWHI	ERE						
ı	_OCATION	EDOM	то		;	2-INCH LF	EXISTING EAC		2-INCH LF											
	TEGORY 001		10				LAC	/11	LI	_										
	ERSECTION																			
		SB13 -				10														
		SB16 -				20														
		PB1 -	CINC VA	JLI			2		50											
INT	TERSECTION	TOTAL				30	2		50	-					TRAFFIC S	IGNAL HEA	<u>DS</u>			
				TRAFFI	IC SIGN	AL STRUCTURE	<u>:S</u>									658.017			658.0416	
				654.010	04	CDV 0060 004	657.0100	657	.0430	657.0425						TRAFFIC 3S	2 SIGIN/		EDESTRIAN GNAL FACE	
				054.010	וע	SPV.0060.001	657.0100			STANDARDS				SIGNAI	_ HEAD		H 1		16-INCH	
				CC	NCRET	TE BASES	PEDESTA		ALUMIN				CATION	BASE N	O. NO.	EACH	1	EACH	EACH	
OCATION / BASE				TYPE 1	1	TYPE 1 SPECIAL	L BASES	10	)-FT	15-FT		CATEGO								
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ATEGORY 0010 ITERSECTION														SB13	17	1				
SB13	6+98.2	11.6	R			1	1			1					21			1		
SB16	6+14.0	0.9	L	1			1		1					SB16	8A				1	
TERSECTION TO	OTAL			1		1	2		1	1		INTERSE	ECTION TO	TAL		1		1	2	
										I										
										PEDESTRI	IAN PUSH E	BUTTONS						TRAFFIC	SIGNAL REMO	VALS
		REMO\	<u>/E TRAFF</u>	IC SIGNA	<u>ALS</u>							658.0500						<u></u>		
					SP\	V.0105.001						PEDESTRIAN								204.0195
						REMOVE						PUSH BUTTON								REMOVING CONCRETE
					TRAFF	FIC SIGNALS				LOCATION / BAS	SE NUMBER									BASES
	1.004	ION			(USH 1	12 & 3RD ST)				CATEGORY 0010		2, (0.1						OCATION / ITE	M NUMBER	EACH
CATEGOR	LOCAT	UN				LS				INTERSECTION							CAT	EGORY 0100		
CATEGOR	INTERSEC	CTION				1				SB11		1					INTE	RSECTION	2	4
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	7080-00-75				HWY: US			COLINIT	Y: EAU CI	LAIRE		MISCELLANEOUS QUA	ΔΝΙΤΙΤΙΕς						SHEE	т

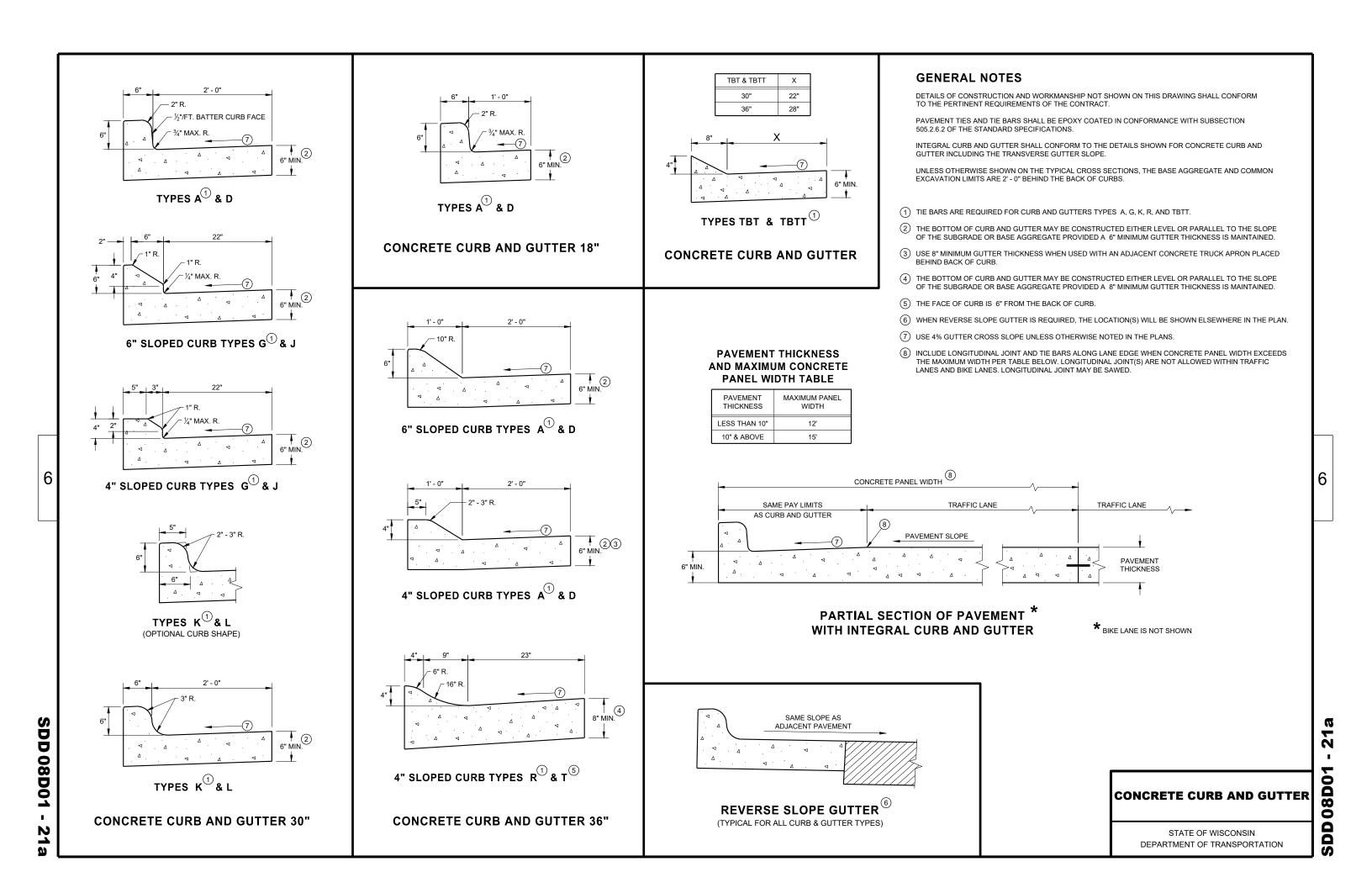
TRAFFIC SIGNAL CABLE AND WIRE - ABOVE GROUND





# Standard Detail Drawing List

08D01-21A	CONCRETE CURB & GUTTER
08D01-21B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09B02-10	CONDUI T
09C02-09	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09E01-15B	POLE MOUNTINGS FOR TRAFFIC SIGNALS AND LIGHTING UNITS, TYPE 3 (HEAVY DUTY)
09E01-15G	HARDWARE DETAILS FOR POLE MOUNTINGS
09E06-05	TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT.
09E07-06	TRAFFIC SIGNAL STANDARD PEDESTRIAN AND FLASHER TYPICAL MOUNTING DETAILS
15C11-07A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D12-09A	TRAFFIC CONTROL, LANE CLOSURE
15D21-07A	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D21-07B	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D27-03	TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-06A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-06B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-06C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS

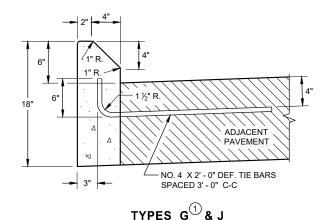


# **DETAIL OF CURB AND GUTTER AT INLETS**

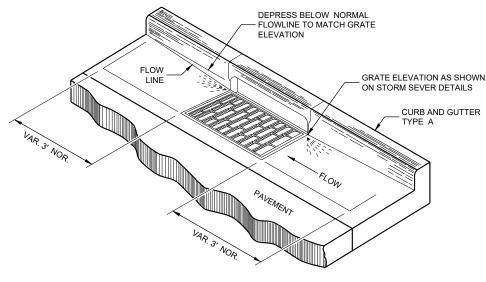
(TYPICAL H INLET COVER SHOWN)

½"/FT. BATTER, FACE OF CURB (ABOVE ADJACENT PAVEMENT) ADJACENT PAVEMENT - NO. 4 X 2' - 0" DEF. TIE BARS

TYPES A D



**CONCRETE CURB** 



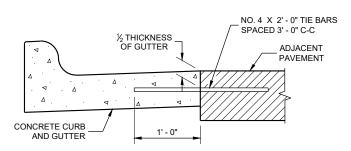
### **GENERAL NOTES**

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

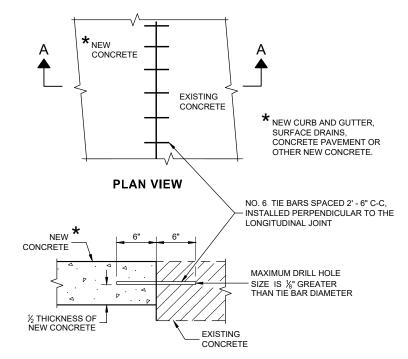
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'- 0" BEHIND THE BACK OF CURBS.

- 1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- 2 THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- 9 REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

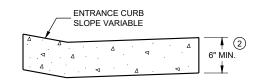


TYPICAL TIE BAR LOCATION  $^{\scriptsize \textcircled{1}}$ 



SECTION A - A

**TIE BARS DRILLED** INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB® (WHEN DIRECTED BY THE ENGINEER)

### **CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS**

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**08DO**,

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
ENGINEER February 2020 DATE

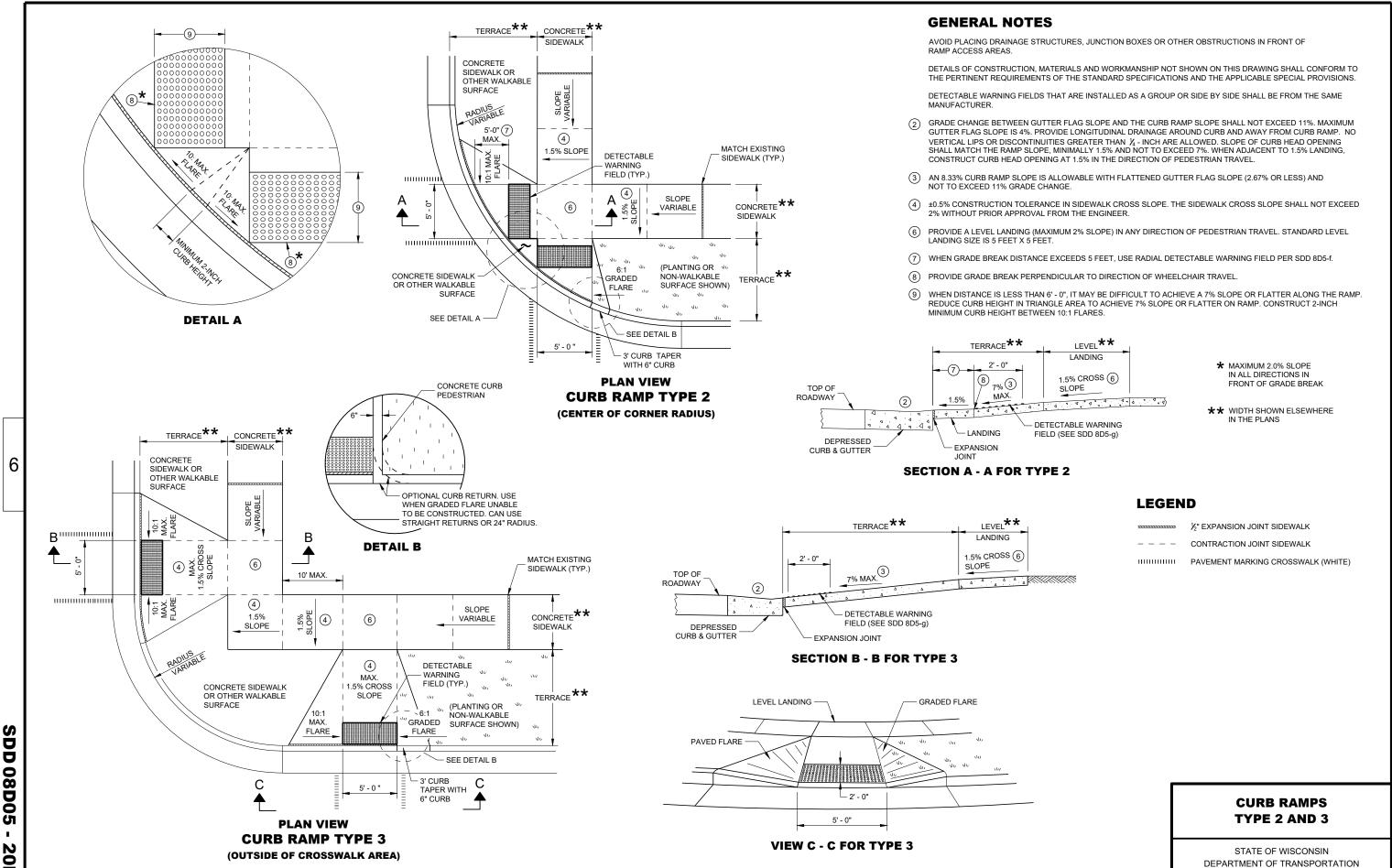
**VIEW D - D FOR TYPE 1 - A** 

**SECTION B - B FOR TYPE 1** 

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STATE OF WISCONSIN

DEPARTMENT OF TRANSPORTATION

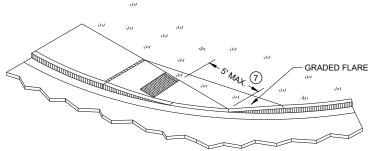


- 20b

.DD 08D05 - 2

**SDD 08D05** 

**ISOMETRIC VIEW FOR TYPE 4A** 



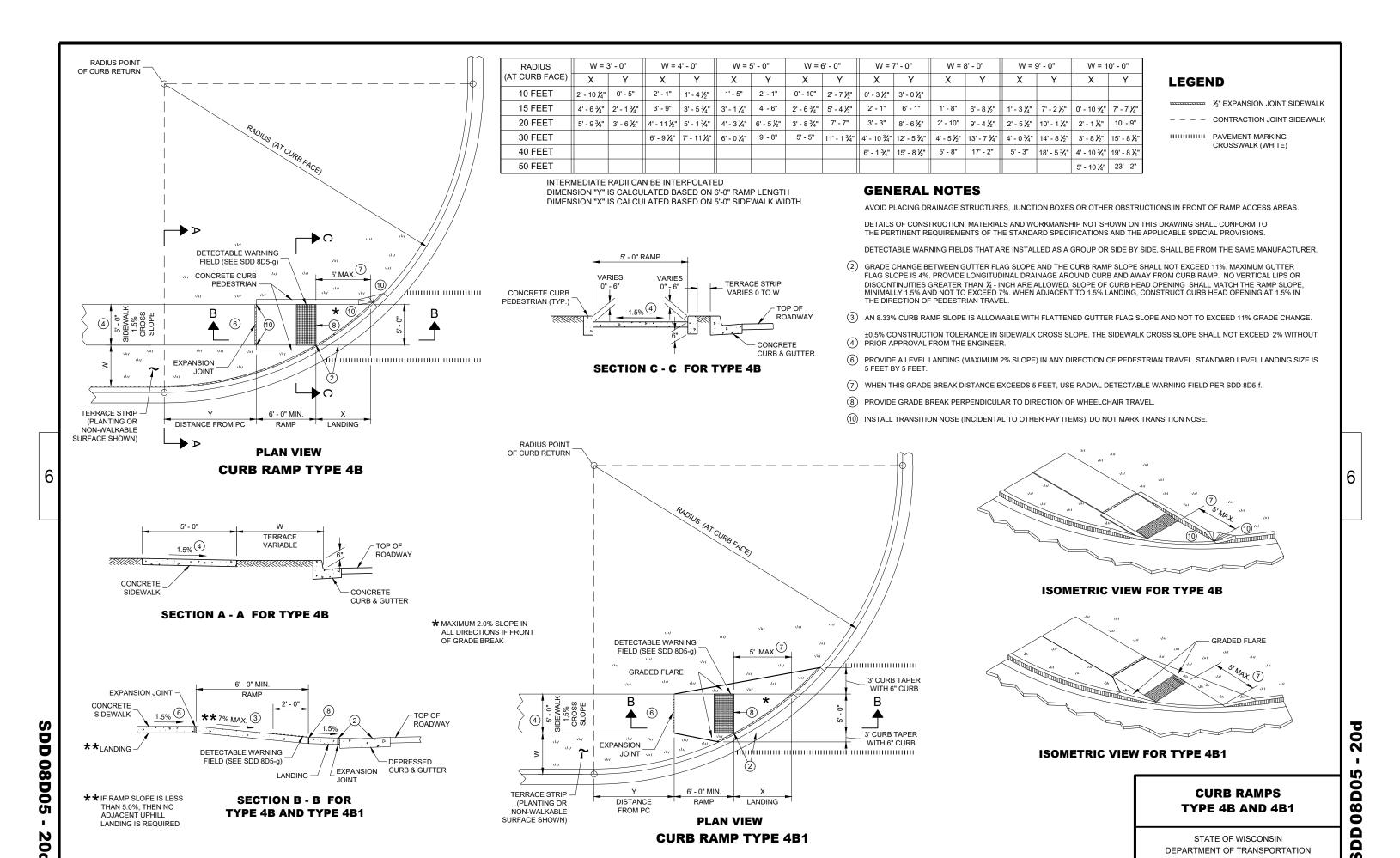
**ISOMETRIC VIEW FOR TYPE 4A1** 

**CURB RAMPS TYPE 4A AND 4A1** 

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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SDD



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08D0

DEPARTMENT OF TRANSPORTATION

**SDD 08D05** 

**DEPRESSED CURB & GUTTER** 

\*\*\* MAXIMUM 8.33%

FIELD (SEE SDD 8D5-a)

**SECTION B - B FOR TYPE 4B1** 

IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO

LANDING IS REQUIRED

ADJACENT UPHILL

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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

**FIELD APPLICATIONS** 

	, A	- IVAN	11
	В		0
A B			0
	PL	AN VIEW	,
<b></b>	c <del> -</del> -		

RAMP



**ELEVATION VIEW** 

#### **TRUNCATED DOMES DETECTABLE WARNING PATTERN DETAIL**

MIN.

1.6"

0.65"

\*

0.9"

★ THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

В

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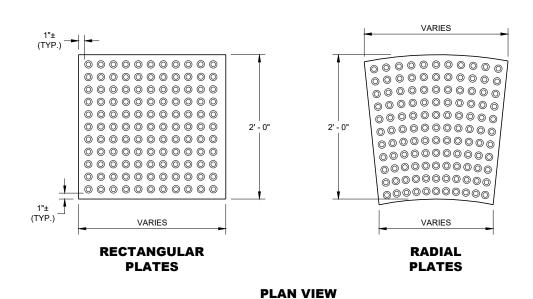
MAX.

2.4"

1.5"

\*

1.4"

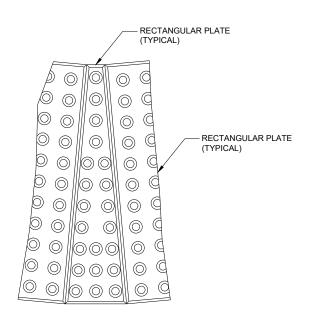


**DETECTABLE WARNING FIELDS (TYPICAL)** 

**PLAN VIEW** RADIAL DETECTABLE **WARNING FIELD ATTRIBUTES** 

RADIAL PLATE

CURB RAMP



**PLAN VIEW RADIAL WEDGE PLATE CONNECTION DETAIL** 

#### **CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES**

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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

/S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR May 2019
DATE

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER. PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION. FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

**GENERAL NOTES** 

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS, PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES, CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

## TYPICAL APPLICATION OF SILT FENCE

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#### PLAN VIEW SILT FENCE AT MEDIAN SURFACE DRAINS



#### **GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- 2 FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- 3 WOOD POSTS SHALL BE A MINIMUM SIZE OF 11/8" X 11/8" OF OAK OR HICKORY.
- 4) SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- (5) CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)



SILT FENCE

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INLET PROTECTION, TYPE A

#### **GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- 1) FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- (2) FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- (3) FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



#### INLET PROTECTION, TYPE C (WITH CURB BOX)

#### **INSTALLATION NOTES**

#### TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

#### TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE, THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

#### INLET PROTECTION TYPE A, B, C, AND D

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02

/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER 6

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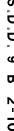
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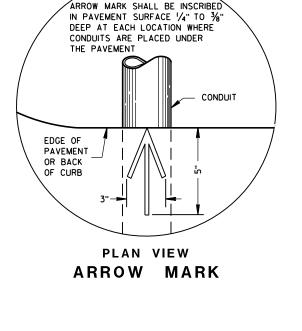


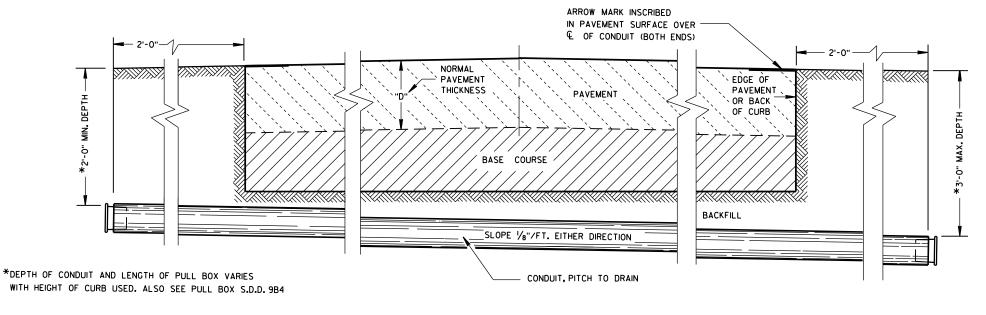












#### SIDE ELEVATION DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

#### **GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSION TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L.LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REIN-STALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

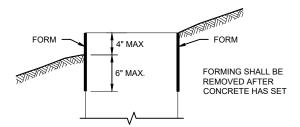
TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

#### CONDUIT

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED	
March, 2017	/S/ Ahmet Demirbilek
DATE	STATE ELECTRICAL ENGINEER



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QUANTITY	CONC	CONCRETE BASE TYPE			
REQUIREMENTS	1	2	5 & 6		
APPROX. CUBIC YARDS OF CONCRET	E 0.40	0.57	0.40		
LBS. OF HOOP BAR STEEL	NONE	23	16		
LBS. OF VERTICAL BAR STEEL	NONE	60	18		

1" CONDUIT

**PURPOSES** 

CONDUIT WITHIN

6" DIA.

ANCHOR RODS SHALL BE

FOR GROUNDING

#### **GENERAL NOTES**

CONDUIT

11 1/2" BOLT CIRCLE

(OUT TO OUT)

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWINGSHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN A THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FRO FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

1" CONDUIT

**PURPOSES** 

6" DIA.

ANCHOR RODS SHALL BE

ORIENTED PARALLEL TO

THE ROADWAY

CONDUIT

11 1/2" BOLT CIRCLE

FOR GROUNDING

CONDUIT WITHIN

CONDUIT

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2. TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH"L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL

WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

- THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.
- (2) (4) 1" DIA. X 3' 6" ANCHOR RODS.
- (3) (4) 1" DIA. X 5' 0" ANCHOR RODS.
- (6) NO. 6 X 6' 8" BAR STEEL REINFORCEMENT.
- (5) (7) NO. 4 X 5' 1" BAR STEEL REINFORCEMENT @ 1' 0" C C.
- (6) (4) 1" DIA. X 3' 6" ANCHOR RODS.
- (6) NO. 4 X 4' 8" BAR STEEL REINFORCEMENT.
- (8) (5) NO. 4  $\times$  5' 1" BAR STELL REINFORCEMENT @ 1' 0" C -C.
- EXOTHERMIC CONNECTION TO EUIPMENT GROUNDING CONDUCTOR
- (10) 5/8" DIA. X 8'-0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED
- ANY ANCHOR ROD PROJECTION SHORTER THAN 2 3/4" OR LONGER THAN 3 1/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- 12) FOR NON BREAKAWAY INSTALLATIONS, 4 ½" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS, RODENT SCREEN REQUIRED.

#### **CONCRETE BASES TYPES 1, 2, 5, & 6**

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED May 2019 DATE STATE ELECTRICAL ENGINEER

CONDUIT CONDUIT WITHIN 12 3/4" BOLT CIRCLE 6" DIA ANCHOR RODS SHALL BE ORIENTED PARALLEL TO THE ROADWAY FORM ALL EXPOSED **HALF SECTION IN HALF SECTION** CONCRETE, PROVIDE 1" CHAMFER ALL AROUND **UNPAVED AREA IN PAVEMENT** (TYPICAL FOR (TYPICAL FOR TYPES 1, 2, 5 & 6) TYPES 1, 2, 5 & 6)

3" (11) TOPSOIL AND SEED OR 3/4" PREFORMED FILLER CRUSHED AS APPROVED BY THE **AGGREGATE ENGINEER** MIN OPTIONAL 4" L BEND

TYPE 1

OR HEX NUT (TYPICAL FOR TYPES 1, 2, 5 & 6

ORIENTED PARALLEL TO (OUT TO OUT) THE ROADWAY FORM ALL EXPOSED CONCRETE. PROVIDE 1" CHAMFER ALL AROUND 3" (11)(12) 6" MIN. 1' - 0" - 3" CLEAR (9) 7' - 0" (5) 10)

OPTIONAL 4" L BEND OR HEX NUT (TYPICAL FOR TYPES 1, 2, 5 & 6 6" MIN

TYPE 2

**CONCRETE BASES** 

FORM ALL EXPOSED CONCRETE. PROVIDE 1" CHAMFER ALL AROUND 3" (11)(12) 6" MIN. 1' - 0" - 3" CLEAR (9) 5' - 0" (8) 10 OPTIONAL 4" L BEND OR HEX NUT (TYPICAL FOR TYPES 1, 2, 5 & 6

6" MIN L 2"

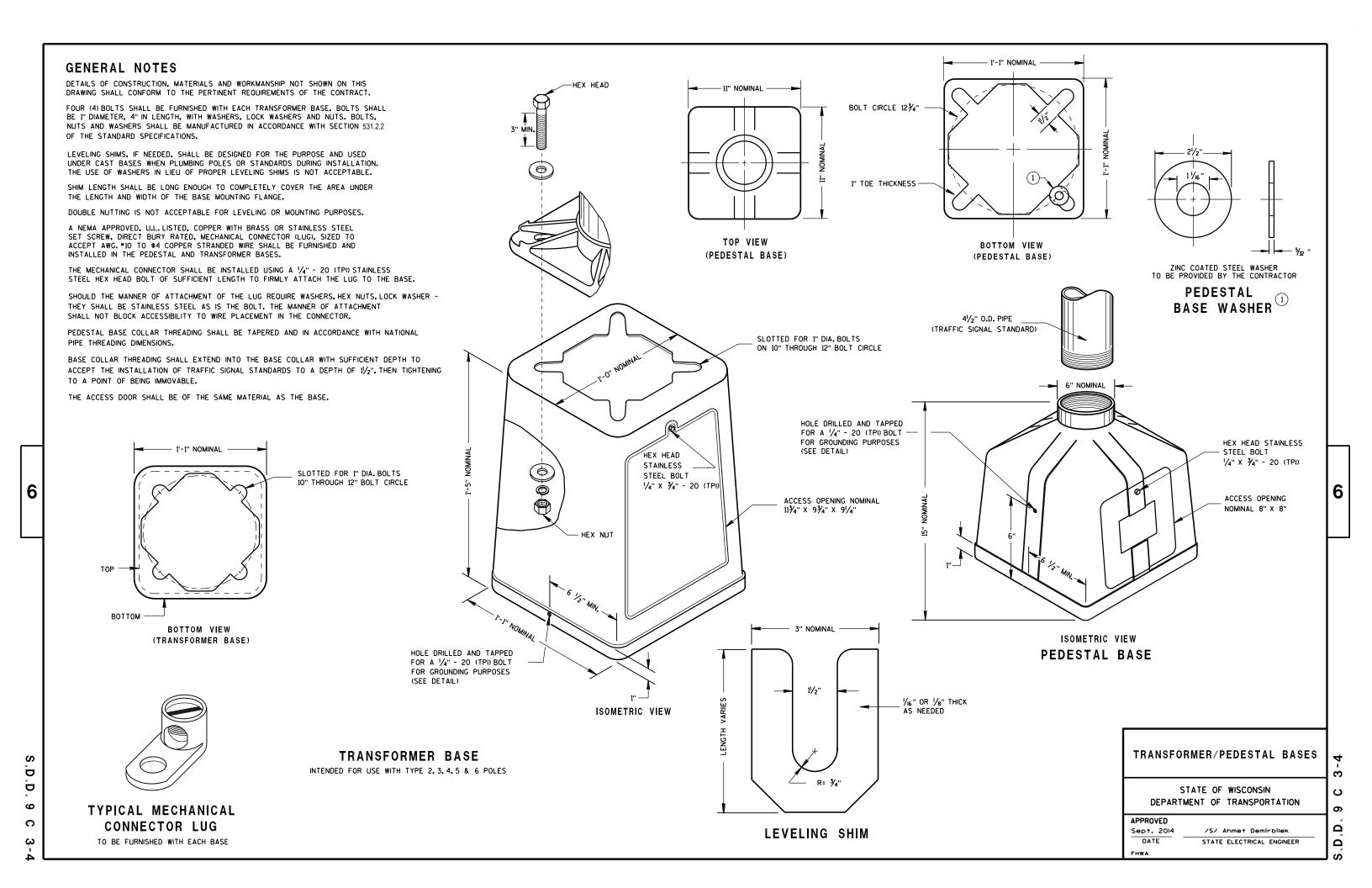
**TYPE 5 & 6** 

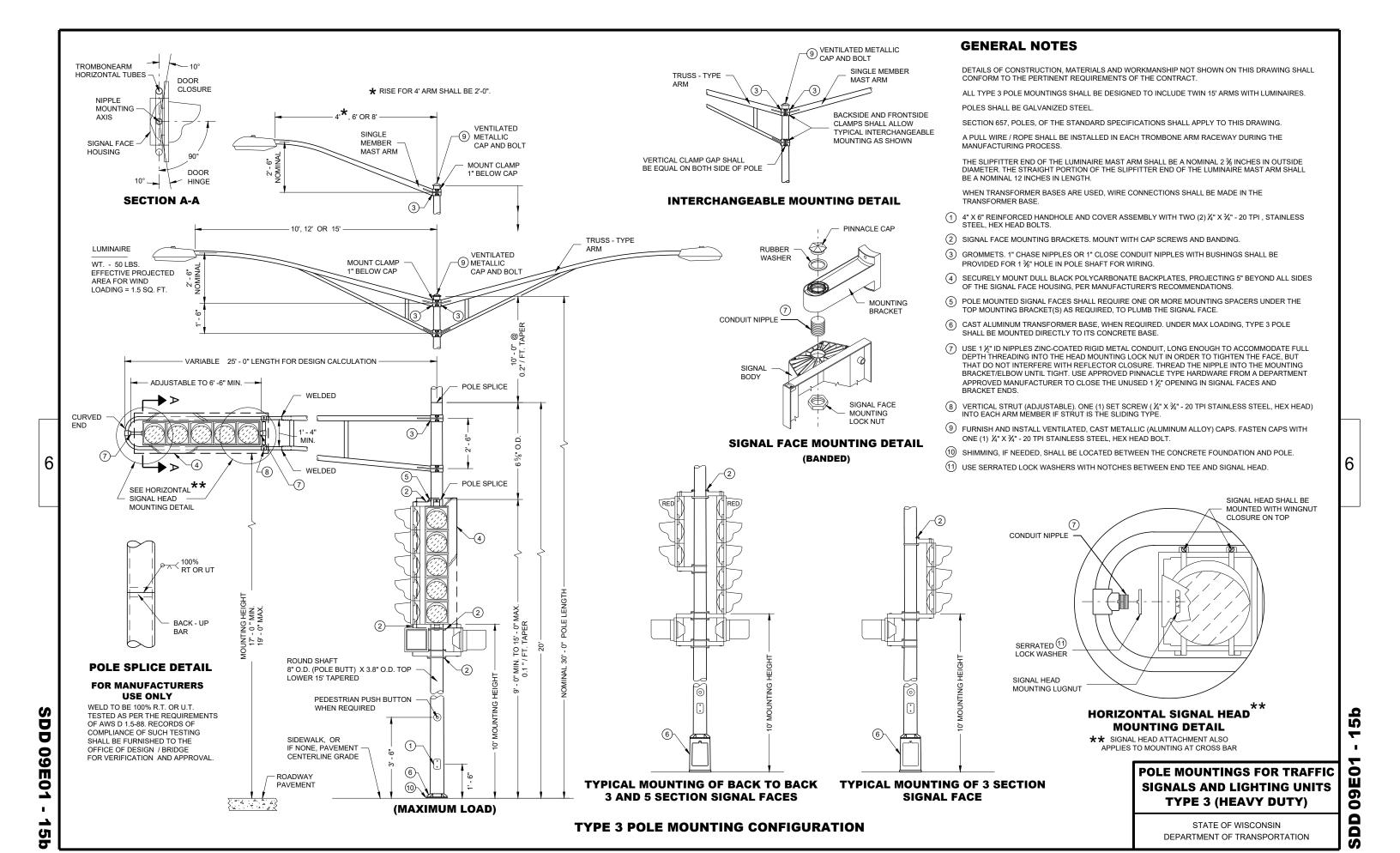
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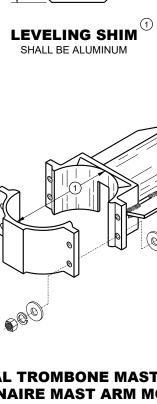
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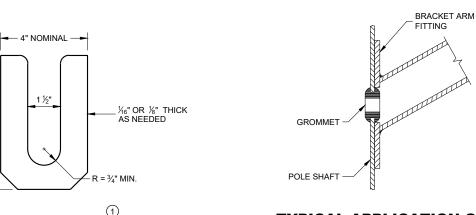
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GUSSETS REQUIRED

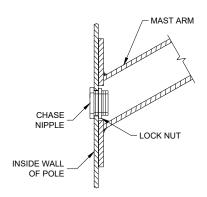
STAINLESS STEEL HARDWARE - BOLT LENGTH

FOR TROMBONE ARM CLAMPS SHALL BE 4 ½"
MIN. - 6" MAX.. BOLTS FOR LUMINAIRE ARM

CLAMPS SHALL BE 3 ½" IN LENGTH. THREAD

BOLTS ENTIRE LENGTH

TYPICAL APPLICATION OF GROMMET IN POLE SHAFT



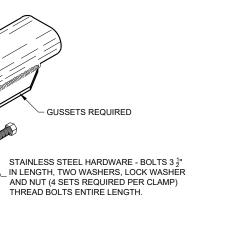
TYPICAL APPLICATION OF CHASE NIPPLE IN POLE SHAFT

#### **GENERAL NOTES**

CLAMP BOLT-NUT TIGHTENING TORQUE SHALL BE INDICATED BY INDENT STAMPING (1/2 INCH NUMERALS AND LETTERS) OR WEATHERPROOF PRINTING ON THE INSIDE OF THE CLAMP THAT IS WELDED TO THE ARM MEMBER.

- (1) 4.5" I.D. FOR LUMINAIRE MAST ARM CLAMP. 6.625" I.D. FOR TROMBONE MAST ARM CLAMP.
- (2) INDIVIDUAL BASE PLATE ANCHOR ROD COVERS. (4 REQUIRED)
- BASE PLATE SLOTTED TO ACCEPT 11" THROUGH 12" BOLT CIRCLE USING 1" DIAMETER ANCHOR RODS
- (4) LEVELING SHIMS, DESIGNED FOR THE PURPOSE, SHALL BE USED WHEN PLUMBING POLES. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE. LEVELING SHIMS SHALL BE USED ONLY BETWEEN THE TOP OF THE CONCRETE BASE AND A METALLIC BASE PLATE.

SHIMS SHALL BE LONG ENOUGH AND WIDE ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.



HOOK FACTORY WELDED TO POLE

"J" HOOK

"J" HOOK

180°

270°

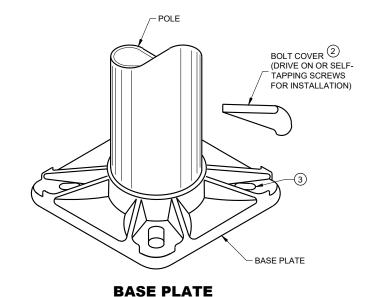
DOOR SIDE

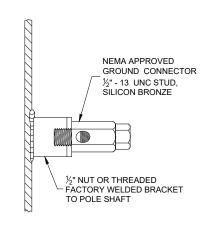
13" RACEWAY HOLE - OPPOSITE DOOR (180° SIDE) IF CALLED FOR

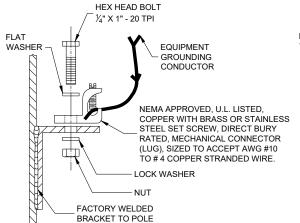
**TYPICAL "J" HOOK LOCATION** 

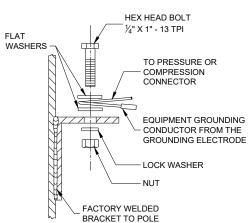
## TYPICAL TROMBONE MAST ARM AND SINGLE LUMINAIRE MAST ARM MOUNTING CLAMP

## TYPICAL LUMINAIRE MAST ARM (DOUBLE) MOUNTING BRACKETS









#### TYPICAL GROUNDING CONNECTIONS

NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL

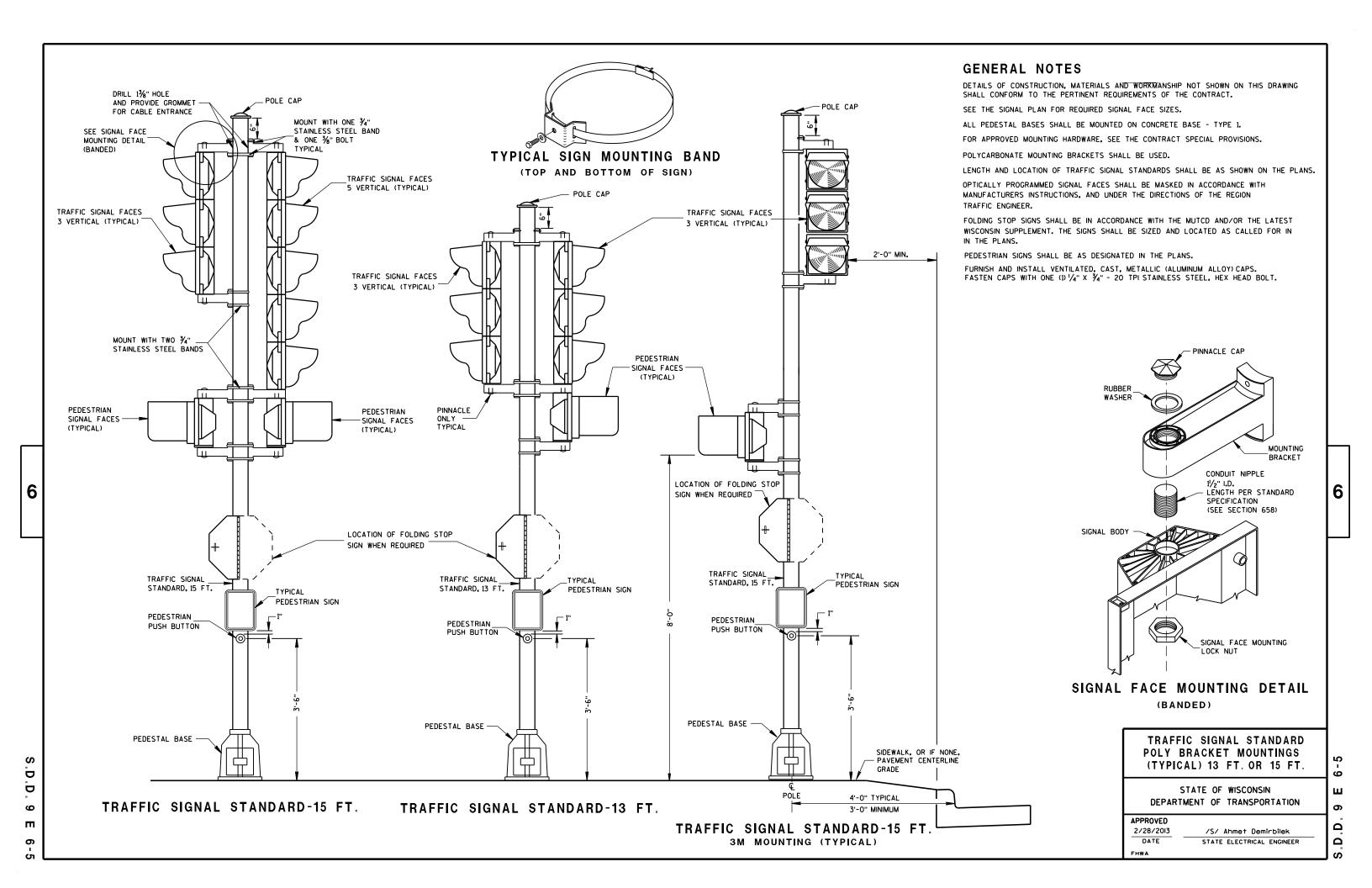
## HARDWARE DETAILS FOR POLE MOUNTING

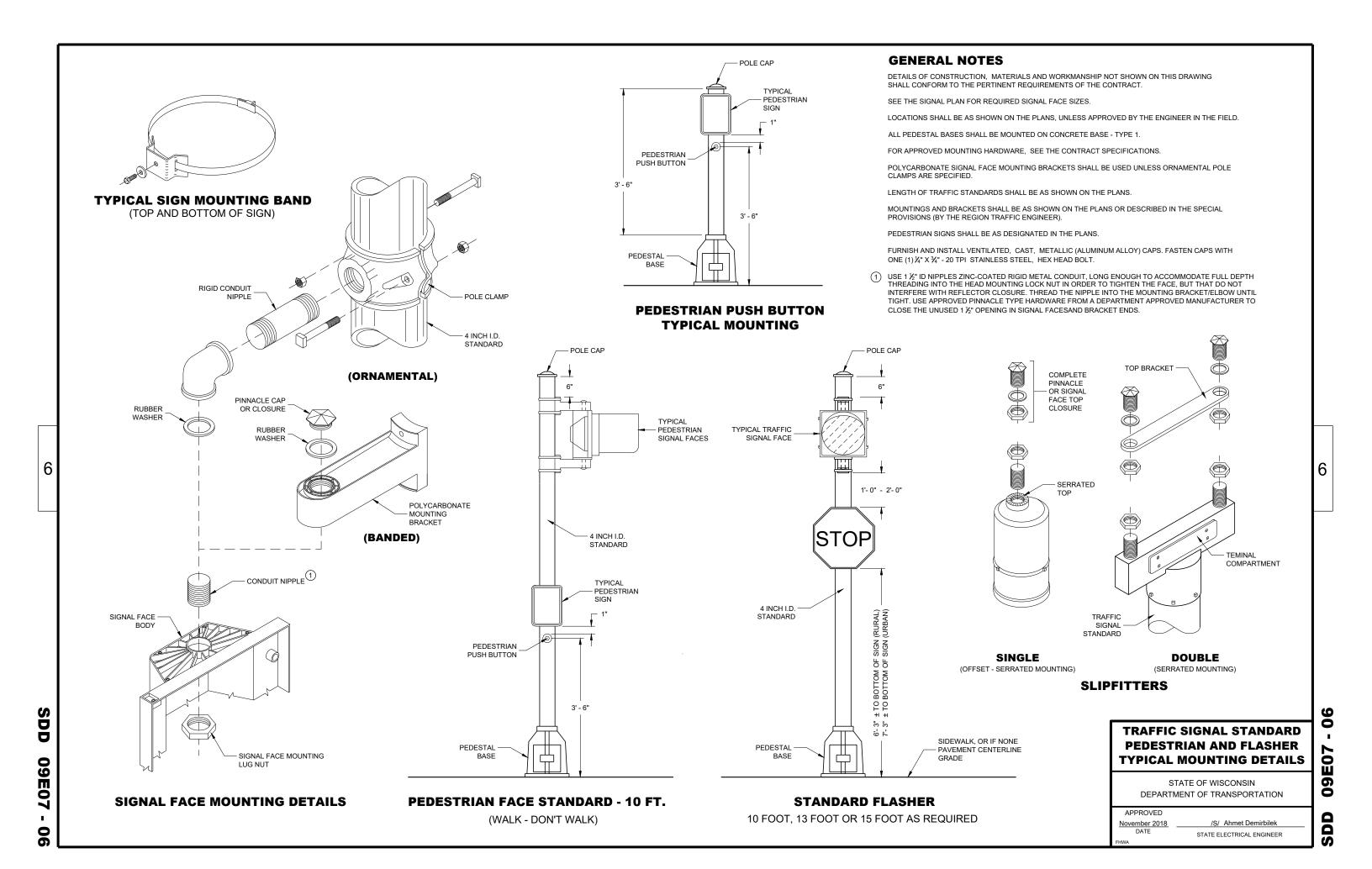
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE

/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGINEER

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DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

**CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST** 

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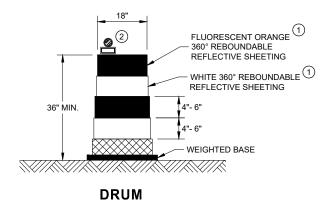
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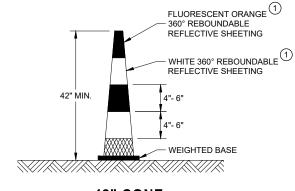
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

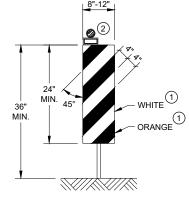
APPROVED June 2017
DATE /S/ Andrew Heidtke WORK ZONE ENGINEER

#### **GENERAL NOTES**

- (1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- (2) LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.





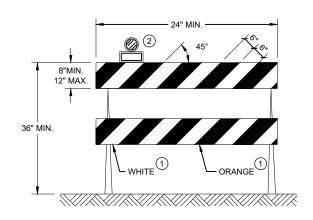


### **42" CONE**

DO NOT USE IN TAPERS ½ SPACING OF DRUMS

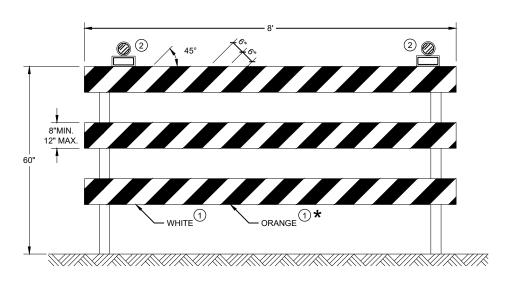
#### **VERTICAL PANEL**

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



#### **TYPE II BARRICADE**

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



#### **TYPE III BARRICADE**

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

#### **CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS**

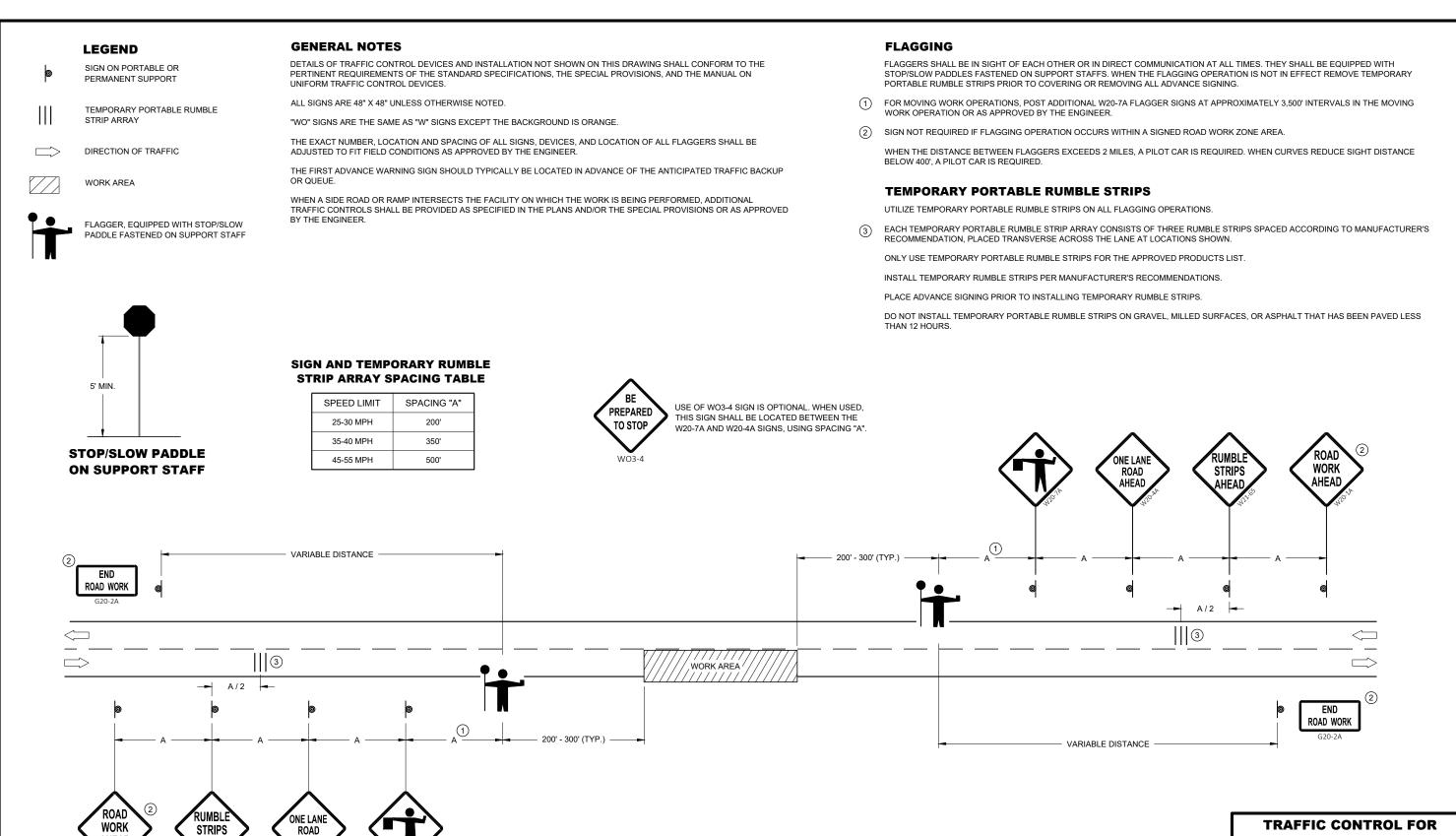
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SDD

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED	
June 2017	/S/ Andrew Heidtke
DATE	WORK ZONE ENGINEER



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

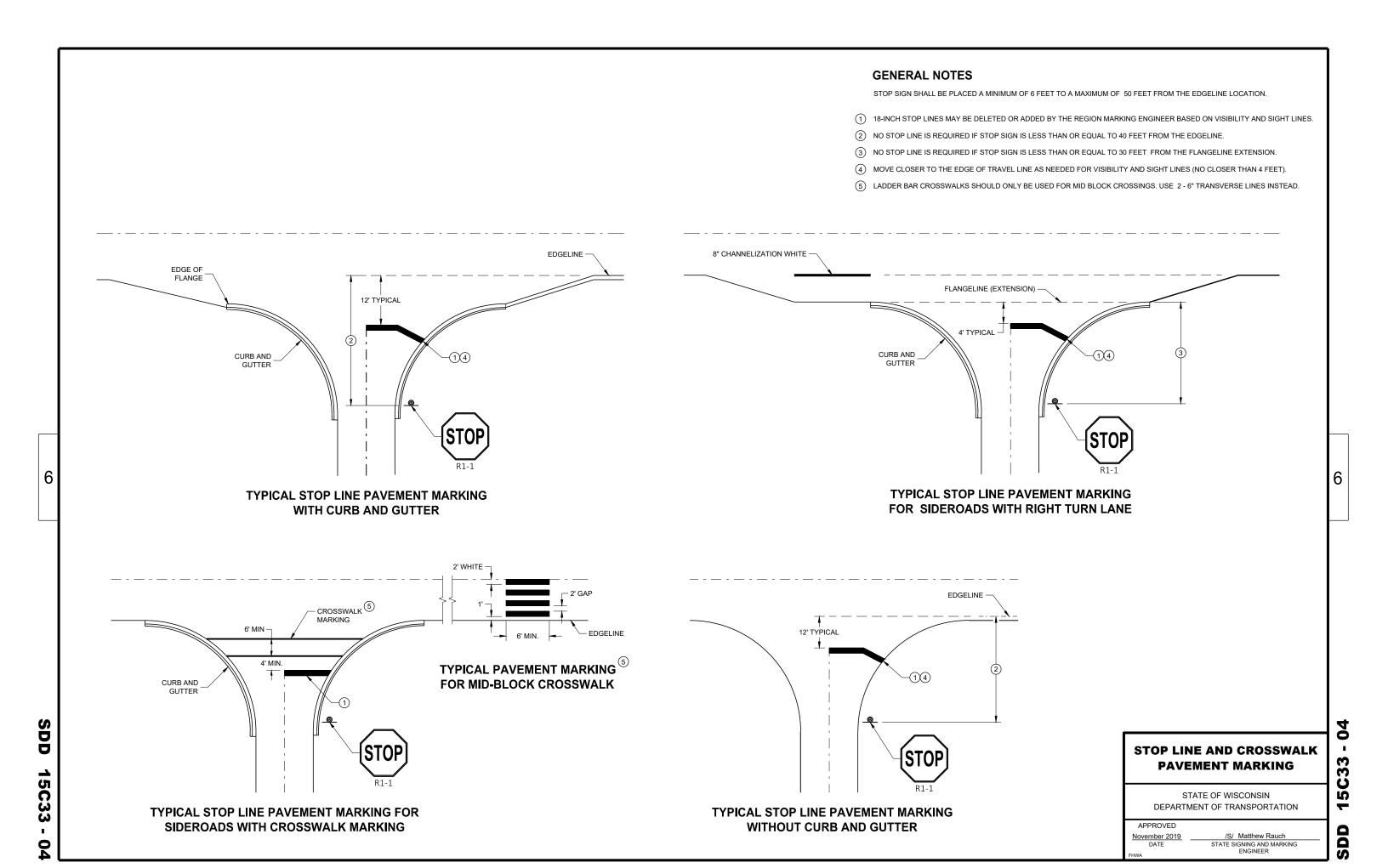
LANE CLOSURE WITH

**FLAGGING OPERATION** 

2

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APPROVED May 2019 DATE WORK ZONE ENGINEER



#### **GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS

NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

#### **LEGEND**

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT

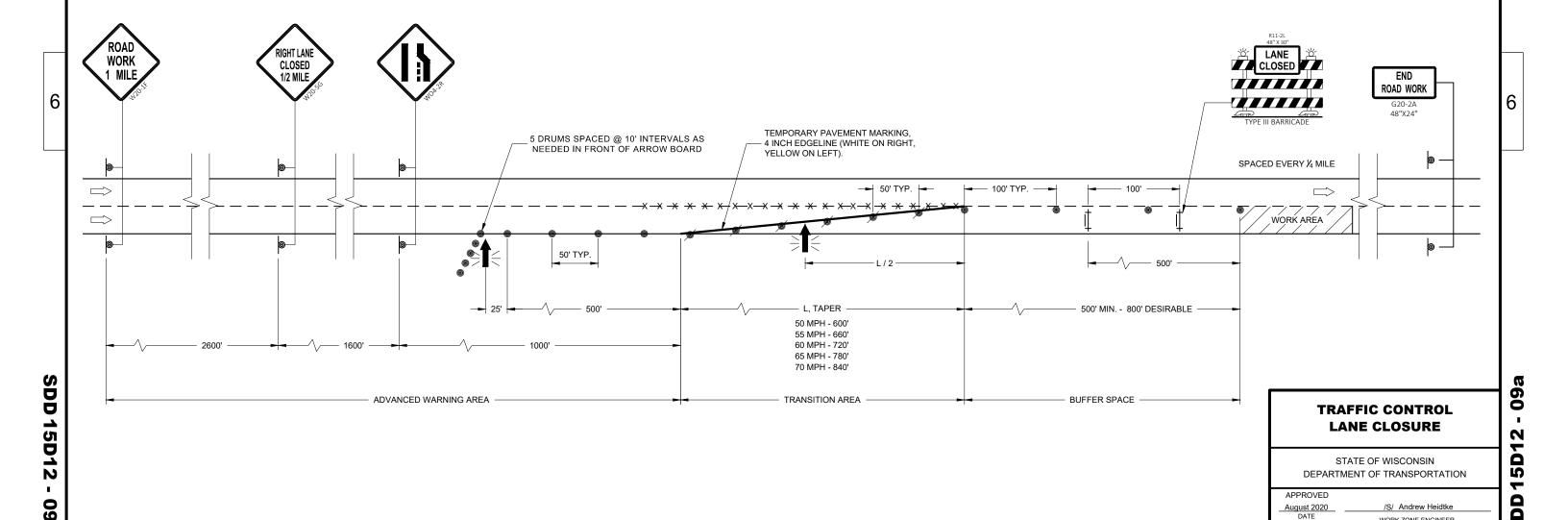
WORK ZONE ENGINEER

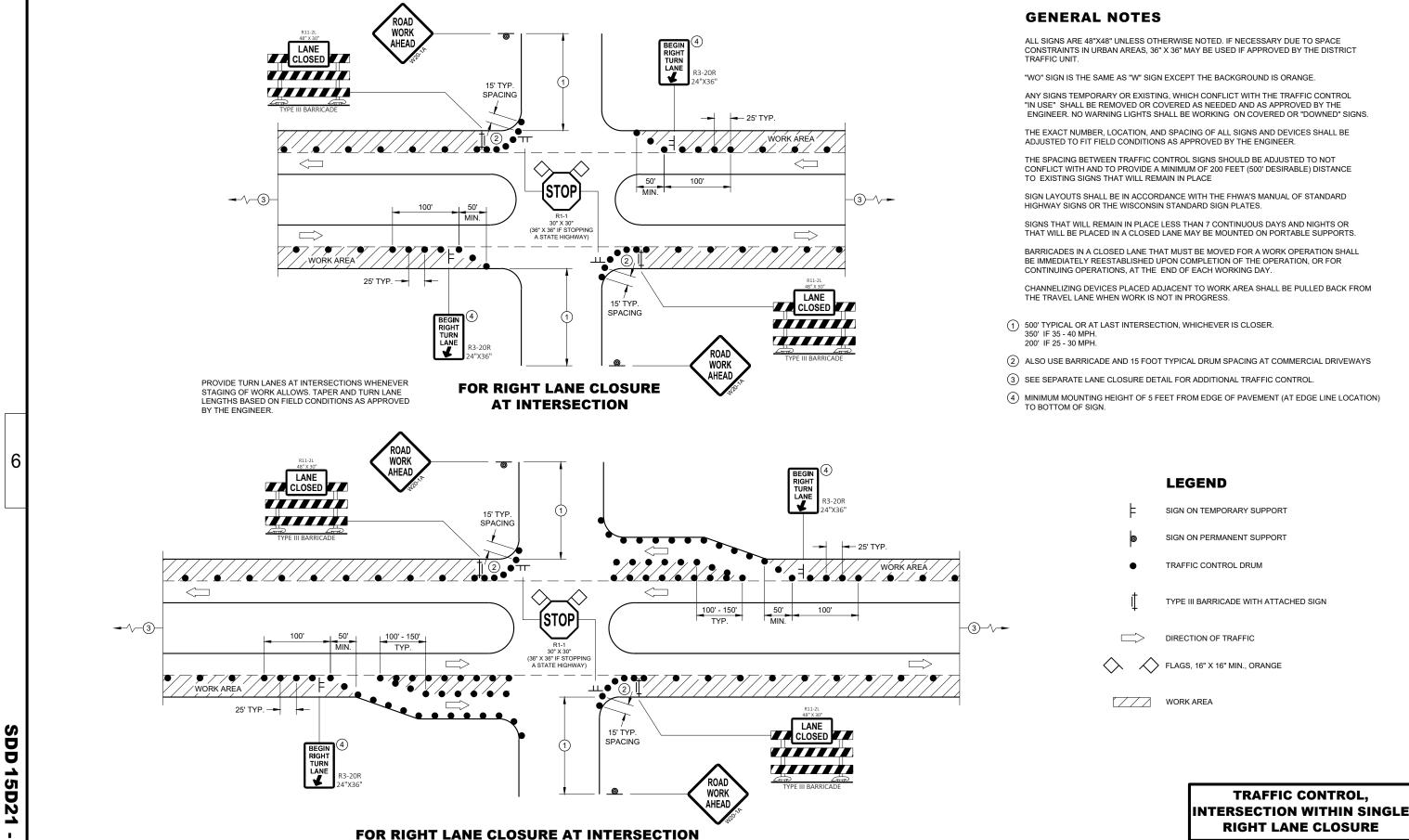
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- -X-X-X- REMOVING PAVEMENT MARKINGS

DIRECTION OF TRAFFIC

WORK AREA

FLASHING ARROW BOARD





(WITH RIGHT TURN BAY OPEN)

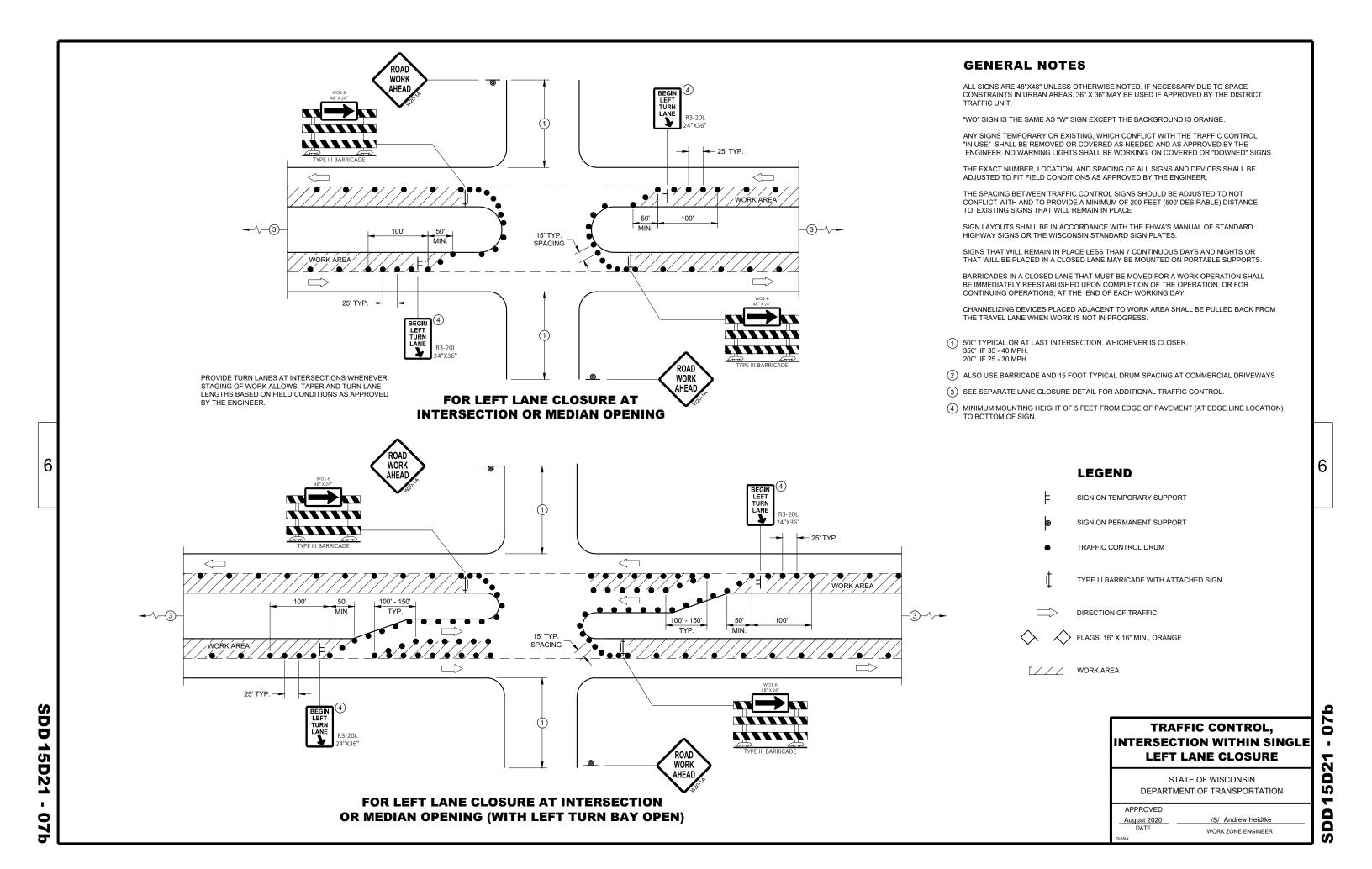
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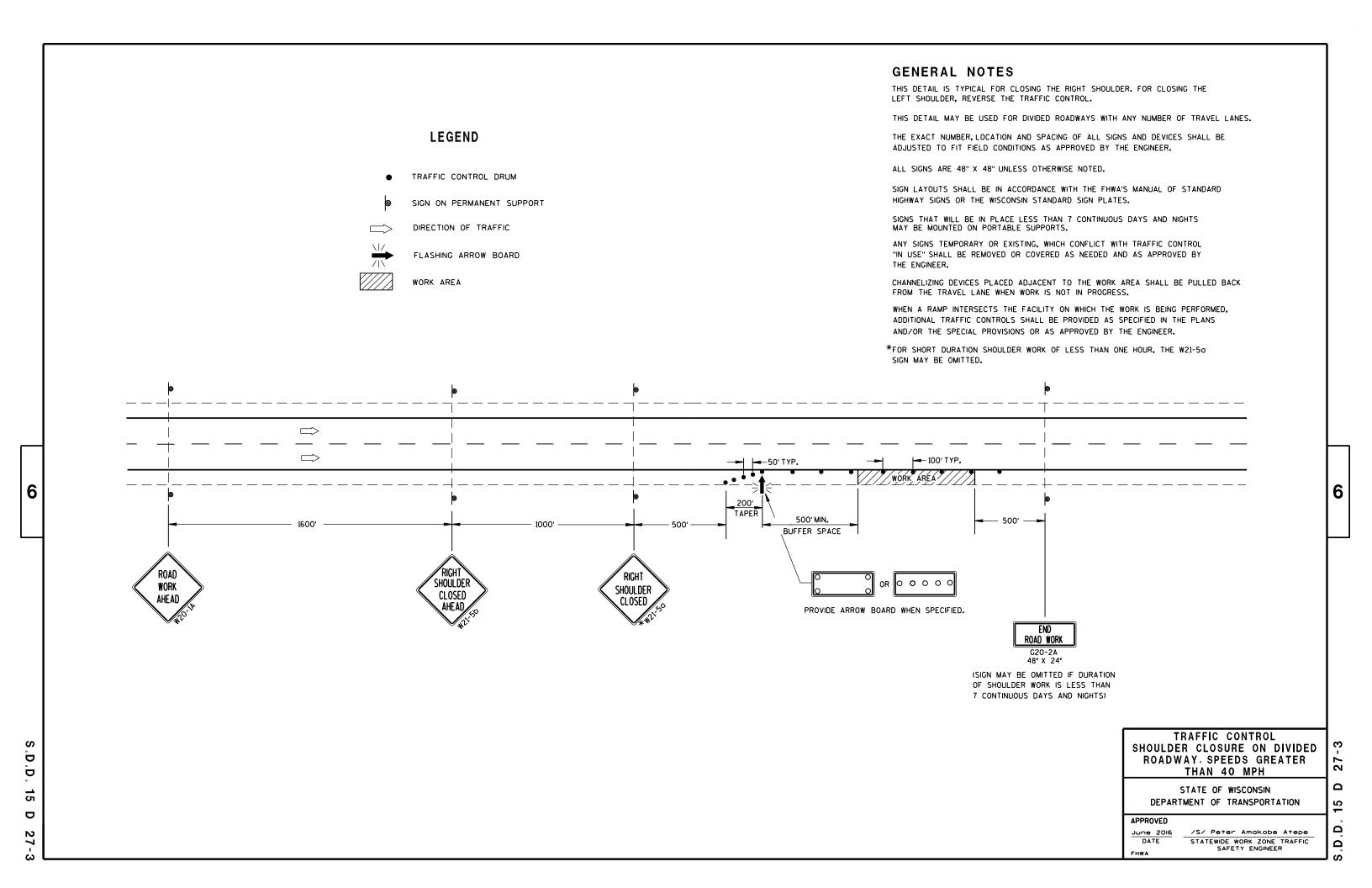
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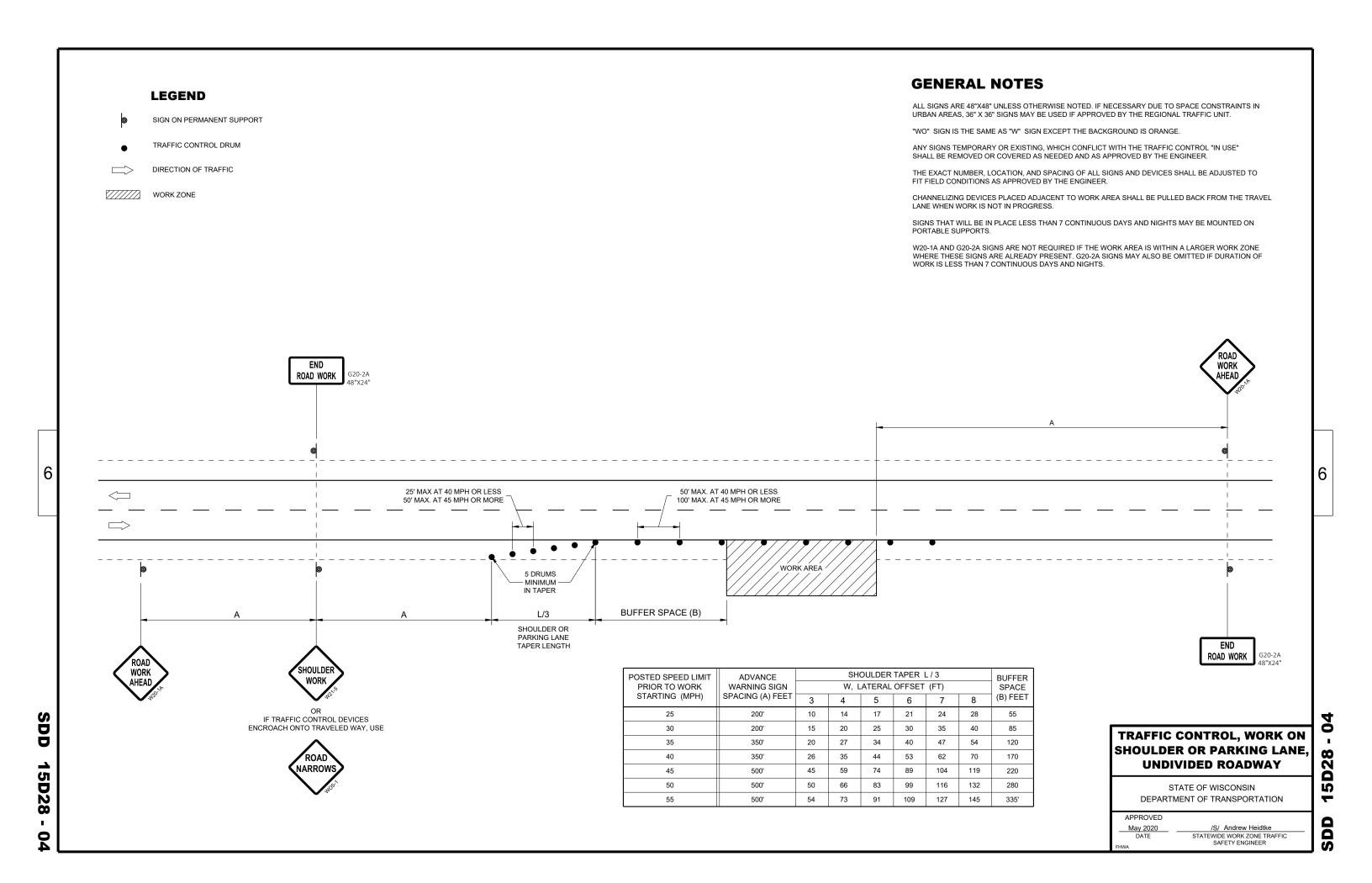
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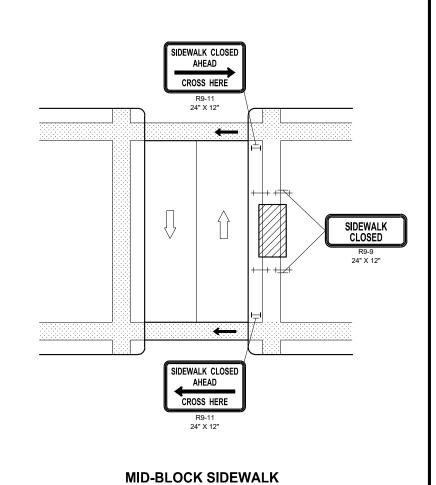
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

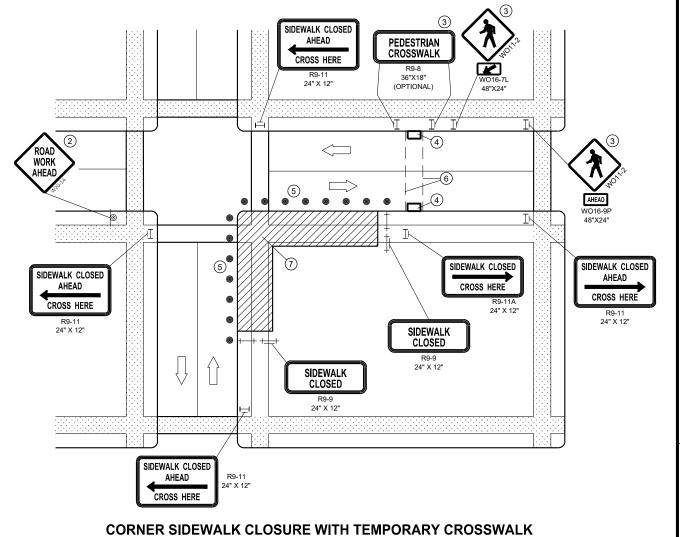






**6**2





#### **GENERAL NOTES**

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

**CLOSURE** 

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN, EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTIME CLOSURE, USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEK LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- 1 IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE
- (2) "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- (3) IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND WO11-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK
- (4) TEMPORARY CURB RAMPS. SEE SDD 15D30 SHEET "b'.
- (5) DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- 6 TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- (7) LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

#### LEGEND

SIGN ON PERMANENT SUPPORT

TRAFFIC CONTROL DRUM

TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)

TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW INTENSITY FLASHING)

[DOCUME] UNDER PEDESTRIAN TRAFFIC

WORK AREA

PEDESTRIAN CHANNELIZATION DEVICE

DIRECTION OF TRAFFIC

## TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

3DD 15D30 - 06a

4" WIDE EDGE MARKING (6)

#### TEMPORARY CURB RAMP PARALLEL TO CURB

CROSS SLOPE 2% MAX. (4)

PROTECTIVE EDGING 2" MIN. HEIGHT

WITH SIDE APRON

ABOVE RAMP SURFACE (2)

9 EDGE TREATMENT 9 EDGE TREATMENT PROTECTIVE EDGING 8 JOINT/GAP TREATMENT (8) JOINT/GAP TREATMENT 2" MIN. HEIGHT (2) CURB -FACE DRAINAGE CURB FACE (2) PROTECTIVE EDGING -2" MIN. HEIGHT ABOVE RAMP SURFACE 4" WIDE EDGE -4" WIDE EDGE -9 45° EDGE CUT -9 45° EDGE CUT -6 MARKING (6) MARKING 1 DETECTABLE -1 DETECTABLE WARNING FIELD WARNING FIELD

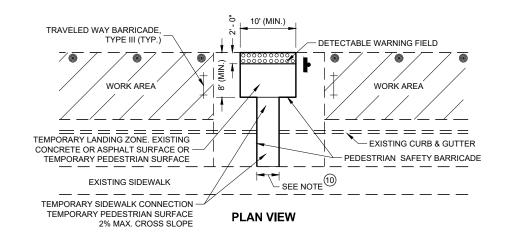
TEMPORARY CURB RAMP PERPENDICULAR TO CURB

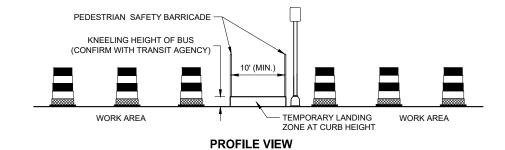
#### **GENERAL NOTES**

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- (1) CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 08D05, SHEET "6".
- (2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- (3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- (4) CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
- (5) CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
- (6) THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
- 7) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- (8) LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.
- (9) CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES SHALL BE VERTICAL UP TO ¼" HIGH AND BEVELED AT 1:2 BETWEEN ¼" AND ½".
- (1) 5" WIDE MIN. WITH PEDESTRIAN SAFETY BARRICADE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY BARRICADE.





#### **TEMPORARY BUS STOP PAD**

#### LEGEND



## TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 15D30 - 06k

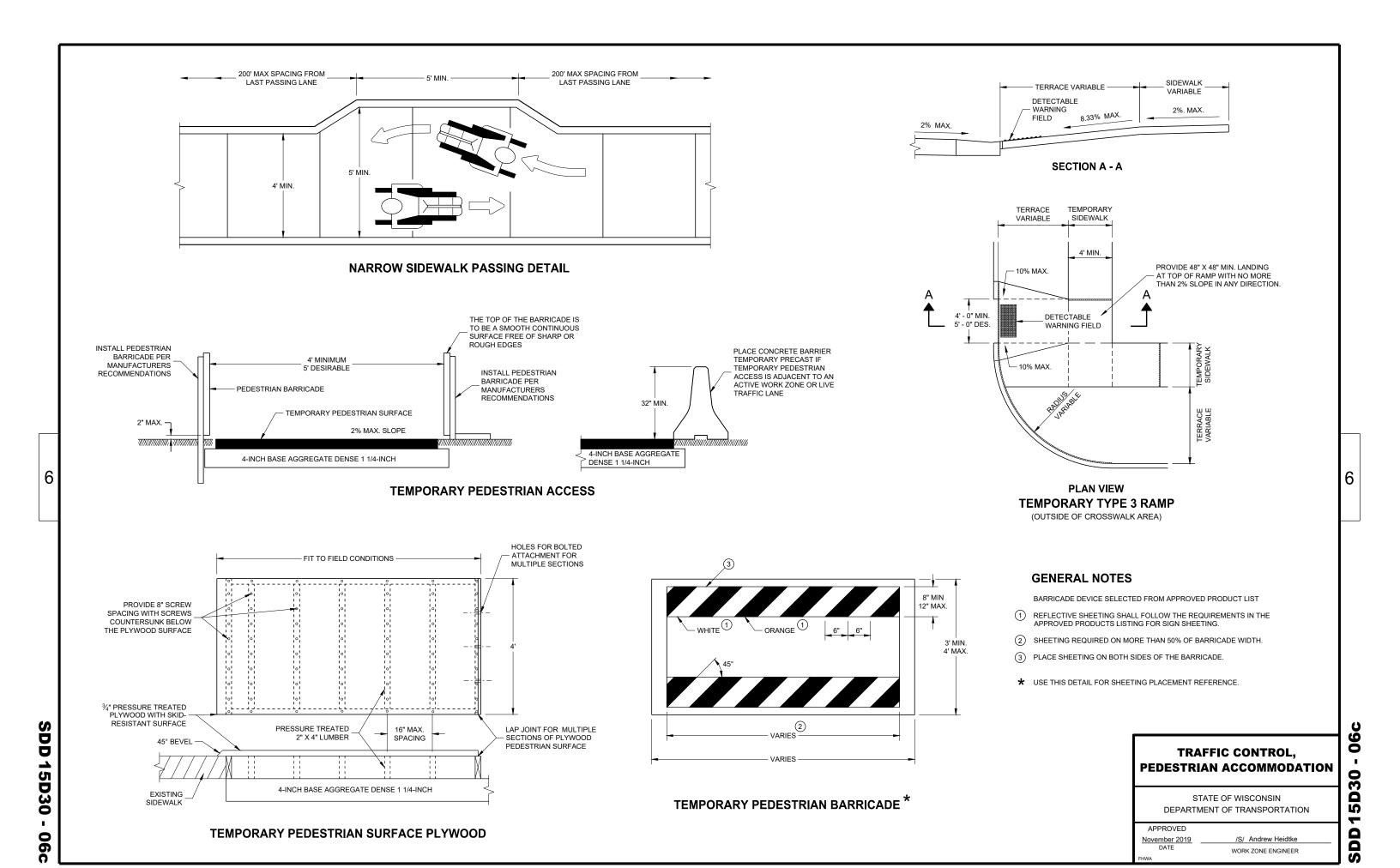
7 DRAINAGE

(5) CLEAR SPACE

(9) EDGE TREATMENT

WITH PROTECTIVE EDGE

SDD 15D30 - 06b





TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SO. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SO.FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE). SIGNS LARGER THAN 27 SO.FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

#### URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST **EMBEDMENT DEPTH** 

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF	
L	E	WOOD POSTS REQUIRED	
48" OR LESS AND LESS THAN 20 SO.FT.	-	1	
LESS THAN 60"	12"	2	٤
60" TO 120"	L/5	2	
GREATER THAN 120" LESS THAN 168"	12"	3	
168" AND GREATER	12"	4	

SEE NOTE (3)

RURAL AREA

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

-11

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- 11/2" DIAMETER HOLES

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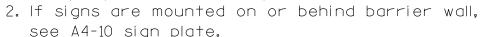
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DEPARTMENT OF TRANSPORTATION

/S/ Andrew Heidtke WORK ZONE ENGINEER

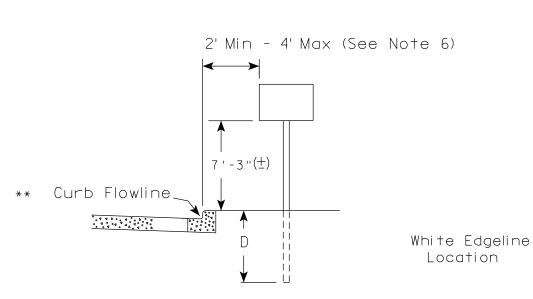
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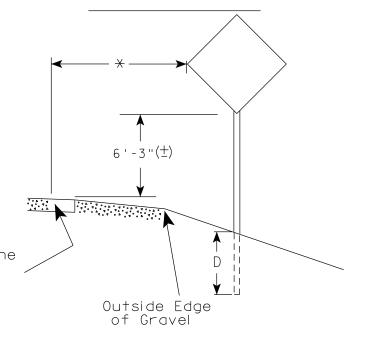
June 2017 DATE



The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52). Mile Markers (D10 series). In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3'' ( $\frac{+}{-}$ ).

- 3. For expressways and freeways, mounting height is  $7'-3''(\pm)$  or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for signs mounted on traffic signal poles is  $5' - 3'' \stackrel{(\pm)}{.}$
- 5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 6. The (+) tolerance for mounting height is 3 inches.
- 7. Folding signs shall be mounted at a height of 5'-3'' ( $\pm$ ) or as directd by the Engineer.





2' Min - 4' Max (See Note 6) 6'-3"(±) \*\* Curb Flowline D

5'-3"(士) White Edgeline  $D \parallel$ Location Outside Edge of Gravel

\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated.

That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
( Sq.Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED For State Traffic Engineer

DATE 5/13/2020 

SHEET NO:

Ε

PROJECT NO: FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\A43.dgn COUNTY:

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: \$\$.....plo†scale.....\$\$ WISDOT/CADDS SHEET 42

PLOT DATE: 13-MAY 2020 1:04



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



#### ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



#### PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

#### GENERAL NOTES

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. J-Assemblies are considered to be one sign for mounting height.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3'' ( $\pm$ ) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4''-3'' (±).
- \* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- \*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- \*\* See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

## POST EMBEDMENT DEPTH

D
(Min)
4'
5'

WISCONSIN DEPT OF TRANSPORTATION APPROVED For State Traffic Engineer DATE 8/21/17 PLATE NO. <u>A4-4.15</u>





	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRE)		
	L	E	
***	Greater than 48" Less than 60"	12"	
	60" to 108"	L/5	

HWY:

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 108" to 144"	12''

COUNTY:

FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 21-AUG-2017 15:54

PLOT SCALE: 108.188297:1.000000

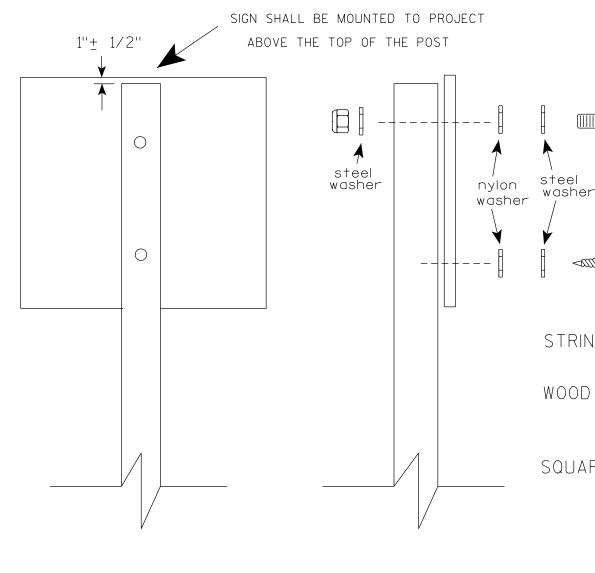
WISDOT/CADDS SHEET 42

OF TYPE II SIGNS ON MULTIPLE POSTS

TYPICAL INSTALLATION

SHEET NO:

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either:

- a. Hot dip galvanized in accordance with ASTM Designation: A 153. Class D. or SC 3
- b. Electro-galvanized in accordance with ASTM Designation: B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS -  $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts

WOOD POSTS  $(4'' \times 6'')$ 

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN) 3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN) 3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 3/32 " (6605-9-6) BULB-TITE. TRI-FOLD. ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ " STEEL 1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X .080 NYLON

Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq.ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

APPROVED

DATE 4/1/2020

PLATE NO. <u>A4-8.9</u>

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\A48.DGN

PROJECT NO:

PLOT DATE: 01-APRIL-2020

PLOT BY : dotc4c

WISDOT/CADDS SHEET 42

Ε

WISCONSIN DEPT OF TRANSPORTATION

Matther ≠or State Traffic Engineer

SHEET NO:



PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

For State Traffic Engineer



## BANDING



SINGLE SIGN





# WASHER PLACEMENT



HWY:

WASHERS (ALL POSTS) -

1-1/4" O.D. X<sup>3</sup>/<sub>8</sub>" I.D. X<sup>1</sup>/<sub>16</sub>" STEEL 1-1/4" O.D.  $\times \frac{3}{8}$ " I.D.  $\times$  .080 NYLON FOR ALL TYPE H SIGNS

CHANNEL

#### GENERAL NOTES

- 1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
- 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
- 3. Banding and assembly bracket shall be stainless steel. All bands shall be  $\frac{3}{4}$ " in width and 0.025" thickness.
- 4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

#### "J" ASSEMBLY



STANDARD SIGN SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

APPROVED

DATE 6/10/19

PLATE NO. A5-9.4

Ε

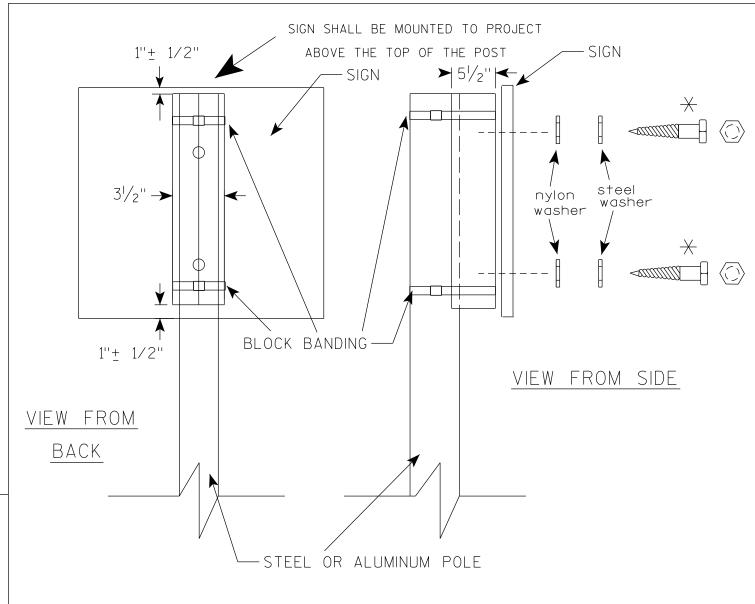
State Traffic Engineer

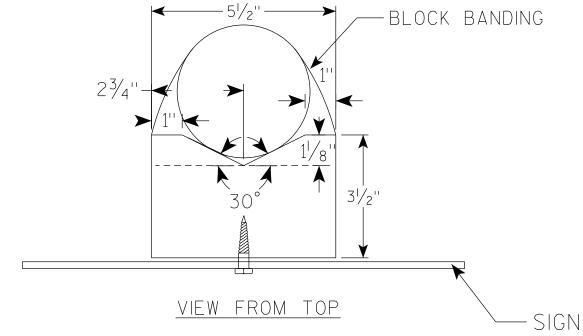
COUNTY:

PLOT NAME :

PLOT SCALE: \$\$.....plotscale.....\$\$ WISDOT/CADDS SHEET 42

PROJECT NO:





### GENERAL NOTES

- 1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
- 2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL,  $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
- 3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS.

  SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
- 4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
- 5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM Designation: B 633, TYPE III, SC 3
- 6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
- 7. STEEL WASHERS SHALL BE  $1\frac{1}{4}$ " O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ "
- 8. NYLON WASHERS SHALL BE  $1^{1}/_{4}$ " O.D. X  $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

 $\rightarrow$  LAG BOLTS SHALL BE  $\frac{3}{8}$ " X  $2\frac{1}{2}$ "

BLOCK BANDING DETAIL ( V-BLOCK OPTION )

WISCONSIN DEPT OF TRANSPORTATION

Matthew R

APPROVED

For State Traffic Engineer

SHEET NO:

DATE <u>6/10/19</u>

PLATE NO. <u>A5-10.2</u>

PROJECT NO:

FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\A510.dgn

PLOT DATE: 10-JUN 2019 4:15

PLOT BY : mscj9h

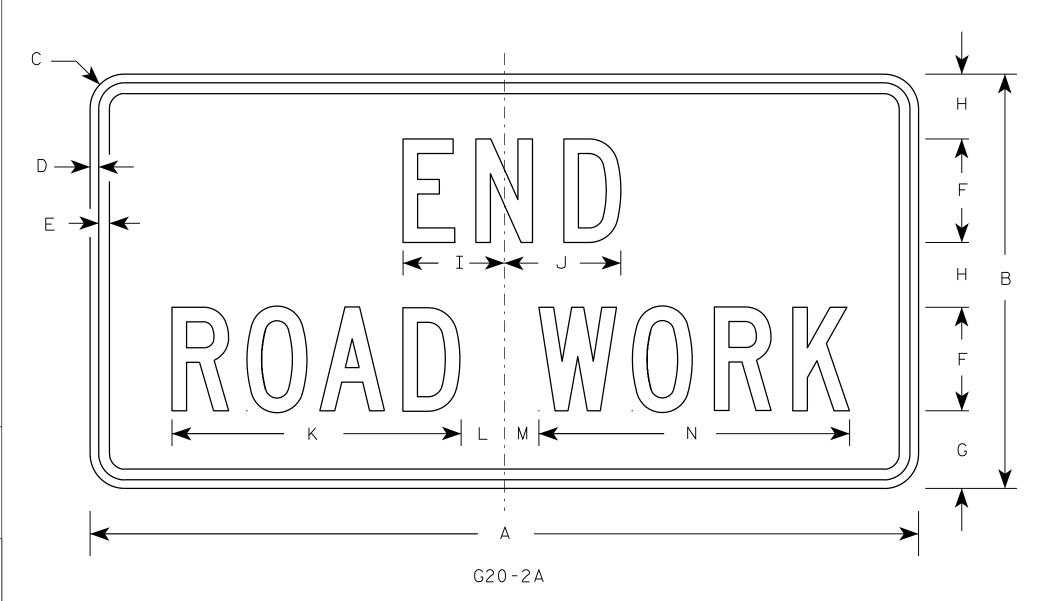
#### NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Metric equivalent for this sign is:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 ½	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 1/8	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72

COUNTY:

STANDARD SIGN G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED AND UN A O N

Matther R Lauch

For State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\G202A.DGN

HWY:

PROJECT NO:

PLOT DATE: 30-SEP-2009 09:31

PLOT BY: ditjph

PLOT NAME :

PLOT SCALE: 5.561773:1.000000



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Red Message - White

3. Message Series - C

<b>*</b>								— А — ;								<b></b>			<b>A</b>	
									H			G —							F	A
		E						               	- 1			_//								<b>Y</b>
D	E	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	Т	U	V	W	Х

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

COUNTY:

STANDARD SIGN R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE <u>11/12/15</u>

PLATE NO. \_\_\_\_R1-1.13

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\R11.DGN

HWY:

PROJECT NO:

PLOT DATE: 22-AUG-2017 07:19

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

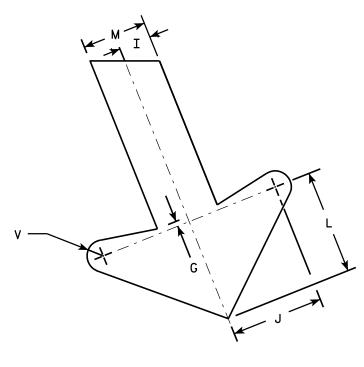
PLOT SCALE: 4.427909:1.000000

#### NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	P	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
25	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 1/8	2 %	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 1/8	2 %	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 %	3	2 1/4	10 1/8	11 1/4		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

COUNTY:

R3-20L

HWY:

М

М

0

STANDARD SIGN R3-20L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew Rauch

For State Traffic Engineer

DATE 10/18/10 PLATE NO. R3-20L.7

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\R320L.DGN

PROJECT NO:

PLOT DATE: 15-OCT-2010 14:45

PLOT BY: dotsja

PLOT NAME :

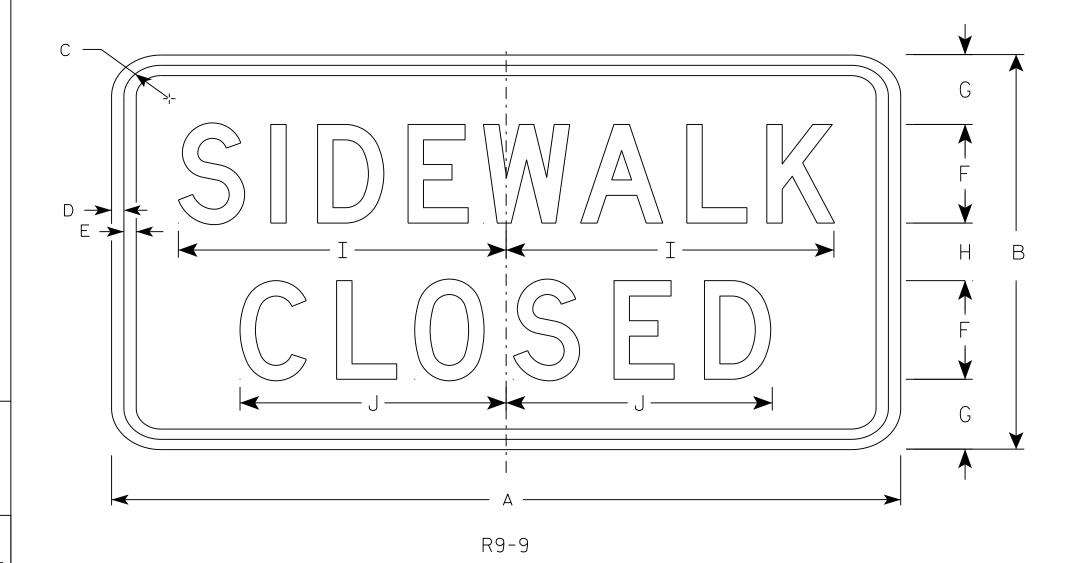
PLOT SCALE: 5.959043:1.000000

#### NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



SIZE A 2S 24 1 3/4 1/2 2 1/8 1 3/4 10 1/2 12 3 8 1/8 2.0 24 1 3/4 1/2 2 1/8 1 3/4 8 1/8 12 10 2.0 1 3/4 3 1/2 30 18 1/2 1/2 3 | 12 1/2 | 10 1/4 3.75

COUNTY:

STANDARD SIGN R9-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Marther R Ray

DATE <u>8/11/16</u>

SHEET NO: R9-9.6

Ε

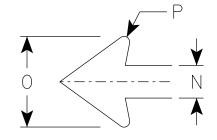
HWY:

PROJECT NO:

- 1. Sign is Type II Type H Reflective
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Use Size 2 for Sidewalks. Use Size 3 for paths and Trails.



ARROW DETAIL

F	
II SIDEWALK CLOSED II \$	
	     B
E N K K T K T K T K T K T K T K T K T K T	

SIZE	А	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	۵	R	S	Т	U	٧	W	X	Y	Z	Area sq. ft.
1																											
25	24	12	1 1/8	3/8	3/8	2	10 1/4	5/8	1 1/2	8 1/4	9 1/4	7	5 %	1	2 3/4	1/8											2.0
2M	24	12	1 1/8	3/8	3/8	2	10 1/4	5/8	1 1/2	8 1/4	9 1/4	7	5 5/8	1	2 3/4	1/8											2.0
3	30	15	1 1/8	3/8	1/2	2 1/2	12 3/4	1/2	2	10 1/4	12 3/8	8	6 3/4	1 1/4	3 %	1/4											3.125
4																											
5																											

COUNTY:

STANDARD SIGN R9-11A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

MM / / / State Traffic Enginee

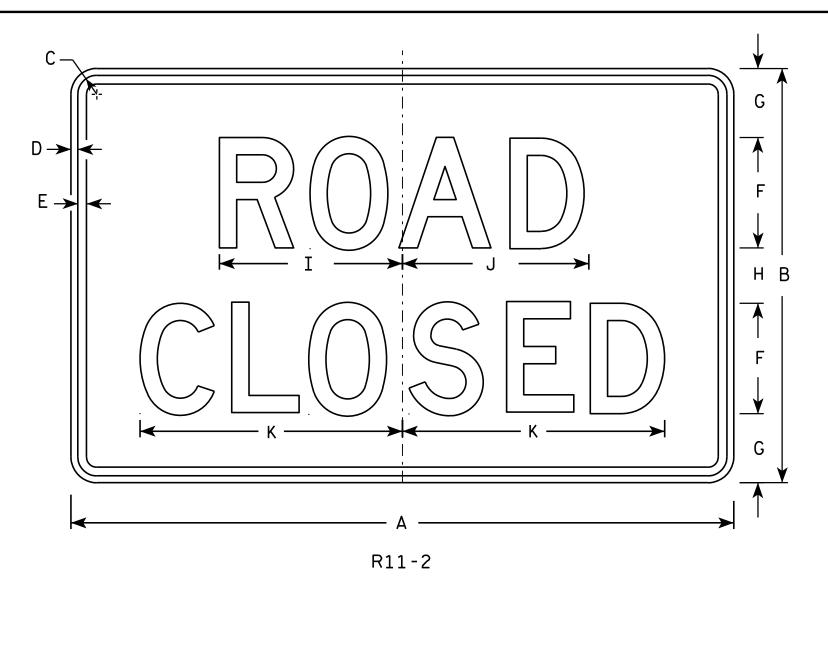
DATE 4/7/2020

PLATE NO. <u>R9-11A.4</u>

SHEET NO: **E** 

HWY:

PROJECT NO:

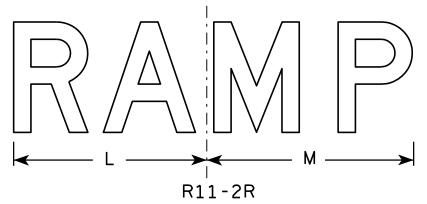


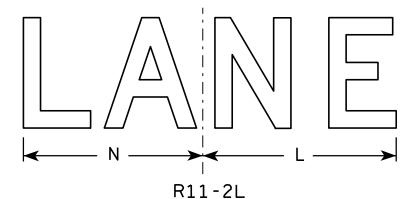
## <u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Modify the message as required.





SIZE	A	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Ρ	0	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
PRO	DJECT	NO:						HWY:					С	OUNTY	<b>':</b>												

STANDARD SIGN R11-2

WISCONSIN DEPT OF TRANSPORTATION

DATE 4/1/11 PLATE NO. R11-2.10

SHEET NO:

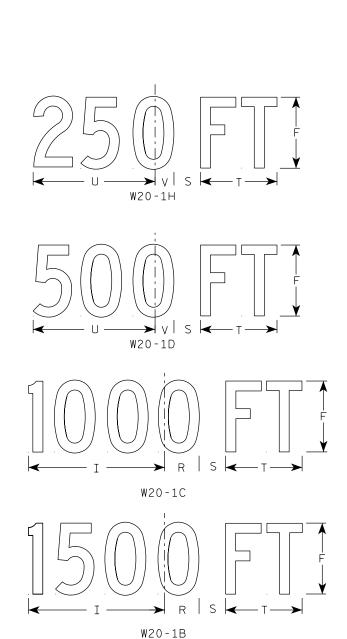
PLOT BY: mscj9h

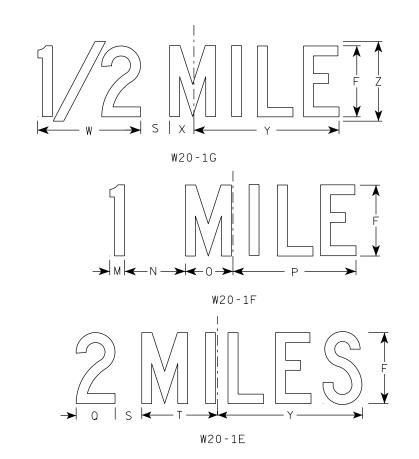
#### NOTES

- 1. Sign is Type II Type F Reflective
- 2. Color:

Background – Orange Message – Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown.
  When base material is metal, the corners and borders shall be rounded.





SIZE	А	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	Q	R	S	Т	U	٧	W	Х	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 1/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 %	9	1 3/8	8	1 3/4	10 3/4	6	9.0
25	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1	6 %	5 3/8	13 1/8	4 3/8	3 1/8	3	8 %	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 1/8	5 3/8	13 1/8	4 3/8	3 1/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1	6 %	5 3/8	13 1/8	4 3/8	3 1/8	3	8 %	13 ¾	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 1/8	5 3/8	13 1/8	4 3/8	3 1/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 1/8	5 3/8	13 1/8	4 3/8	3 1/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew & Paulo

For State Traffic Engineer
DATE 3/25/2020 PLATE NO. W20-1.11

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\W201.DGN

PROJECT NO:

W20-1A

PLOT DATE: 25-MARCH-2020

PLOT BY : dotc4c

WISDOT/CADDS SHEET 42

## NOTES

- 1. Sign is Type II Type F Reflective
- 2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

A
SHOULDER HOLDER
W21-5

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	Q	R	S	Т	U	٧	W	Х	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	3 1/2	16	9																		9.0
25	48		2 1/4	3/4	1	8	5	21 3/8	11 1/4																		16.0
2M	48		2 1/4	3/4	1	8	5	21 3/8	11 1/4																		16.0
3	48		2 1/4	3/4	1	8	5	21 3/8	11 1/4																		16.0
4	48		2 1/4	3/4	1	8	5	21 3/8	11 1/4																		16.0
5	48		2 1/4	3/4	1	8	5	21 3/8	11 1/4																		16.0

COUNTY:

STANDARD SIGN W21-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew & Rawh

For State Traffic Engineer

DATE <u>4/30/2020</u> PLATE NO. <u>W21-5.6</u>

Ε

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\W215.dgn

HWY:

PROJECT NO:

PLOT DATE: 30-APRIL 2020

PLOT BY : dotc4c

PLOT SCALE: \$\$.....plotscale.....\$\$ WISDOT/CADDS SHEET 42

Notes: 1 - Cut

Cut includes Salvaged/Unusable Pavement material

2 - Salvaged/Unusable Pavement Material

This does not show up in cross sections Does not include Unusable Pavement Exc volume or Select Fill

3 - Fill

If Marsh and EBS to be backfilled with Cut or Borrow: [(Cut) - ((Fill - Expanded Rock) \* Fill Factor))] 4 - Mass Ordinate

							;	STAGE 1 (STA 0+	33 - STA (	6+21): PA	TH						
			Arc	ea				Incremental Volu	ıme (Unadjus	ited)				Cumulative Vo	ol (CY)		
								Salvaged/Unusable					Expanded	Expanded Marsh	Reduced Marsh	Reduced EBS	Mass
		Cut	Salvaged/Unusable	Fill	Marsh	EBS	Cut	Pavement Material	Fill	Marsh	EBS	Cut	Fill	Backfill	in Fill	In Fill	Ordinate
STATION	Distance		Pavement Material		Exc		Note 1	Note 2	Note 3	Exc		1.00	1.25	1.50	0.60	0.80	
		(SF)	(SF)	(SF)	(SF)	(SF)	(CY)	(CY)	(CY)	(CY)	(CY)	Note 1					Note 4
0+33		18.9	0.0	0.2	0.0	0.0											
0+50	17	12.4	0.0	0.1	0.0	0.0	10	0	0	0	0	10	0	0	0	0	10
1+00	50	12.8	0.0	0.4	0.0	0.0	23	0	0	0	0	33	1	0	0	0	32
1+50	50	10.3	0.0	0.6	0.0	0.0	21	0	1	0	0	55	2	0	0	0	53
2+00	50	11.9	0.0	0.3	0.0	0.0	21	0	1	0	0	75	3	0	0	0	72
2+50	50	16.3	0.0	0.0	0.0	0.0	26	0	0	0	0	101	3	0	0	0	98
3+00	50	16.7	0.0	1.2	0.0	0.0	31	0	1	0	0	132	5	0	0	0	127
3+50	50	11.0	0.0	5.4	0.0	0.0	26	0	6	0	0	157	12	0	0	0	145
4+00	50	10.6	0.0	3.6	0.0	0.0	20	0	8	0	0	177	23	0	0	0	155
4+50	50	15.8	0.0	5.8	0.0	0.0	24	0	9	0	0	202	33	0	0	0	168
5+00	50	20.6	0.0	8.5	0.0	0.0	34	0	13	0	0	236	50	0	0	0	186
5+50	50	22.5	0.0	9.6	0.0	0.0	40	0	17	0	0	276	71	0	0	0	205
6+00	50	0.0	0.0	40.7	0.0	0.0	21	0	47	0	0	296	129	0	0	0	167
6+14	14	2.7	0.0	6.2	0.0	0.0	1	0	12	0	0	298	144	0	0	0	153
	1						298	0	115	0	0						

			Are	ea				Incremental Volu	ıme (Unadjus	ted)				Cumulative Vo	ol (CY)		
								Salvaged/Unusable					Expanded	Expanded Marsh	Reduced Marsh	Reduced EBS	Mass
		Cut	Salvaged/Unusable	Fill	Marsh	EBS	Cut	Pavement Material	Fill	Marsh	EBS	Cut	Fill	Backfill	in Fill	In Fill	Ordinate
STATION	Distance		Pavement Material		Exc		Note 1	Note 2	Note 3	Exc		1.00	1.25	1.50	0.60	0.80	
		(SF)	(SF)	(SF)	(SF)	(SF)	(CY)	(CY)	(CY)	(CY)	(CY)	Note 1					Note 4
7+66		0.4	0.0	29.4	0.0	0.0											
8+00	34	12.4	0.0	6.2	0.0	0.0	8	0	22	0	0	8	28	0	0	0	-20
8+50	50	17.6	0.0	0.9	0.0	0.0	28	0	7	0	0	36	36	0	0	0	0
9+00	50	21.7	0.0	0.0	0.0	0.0	36	0	1	0	0	72	37	0	0	0	35
9+38	38	20.6	0.0	0.0	0.0	0.0	30	0	0	0	0	102	37	0	0	0	65
							102	0	30	0	0						

9

SHEET Ε PROJECT NO: 7080-00-75 HWY: USH 12 COUNTY: EAU CLAIRE EARTHWORK DATA SHEETS

G:\WDOTNW\19004-002 (USH 12 ALTOONA)\CIVIL 3D\SHEETSPLAN\090101-EW.DWG LAYOUT NAME - 01 FILE NAME :

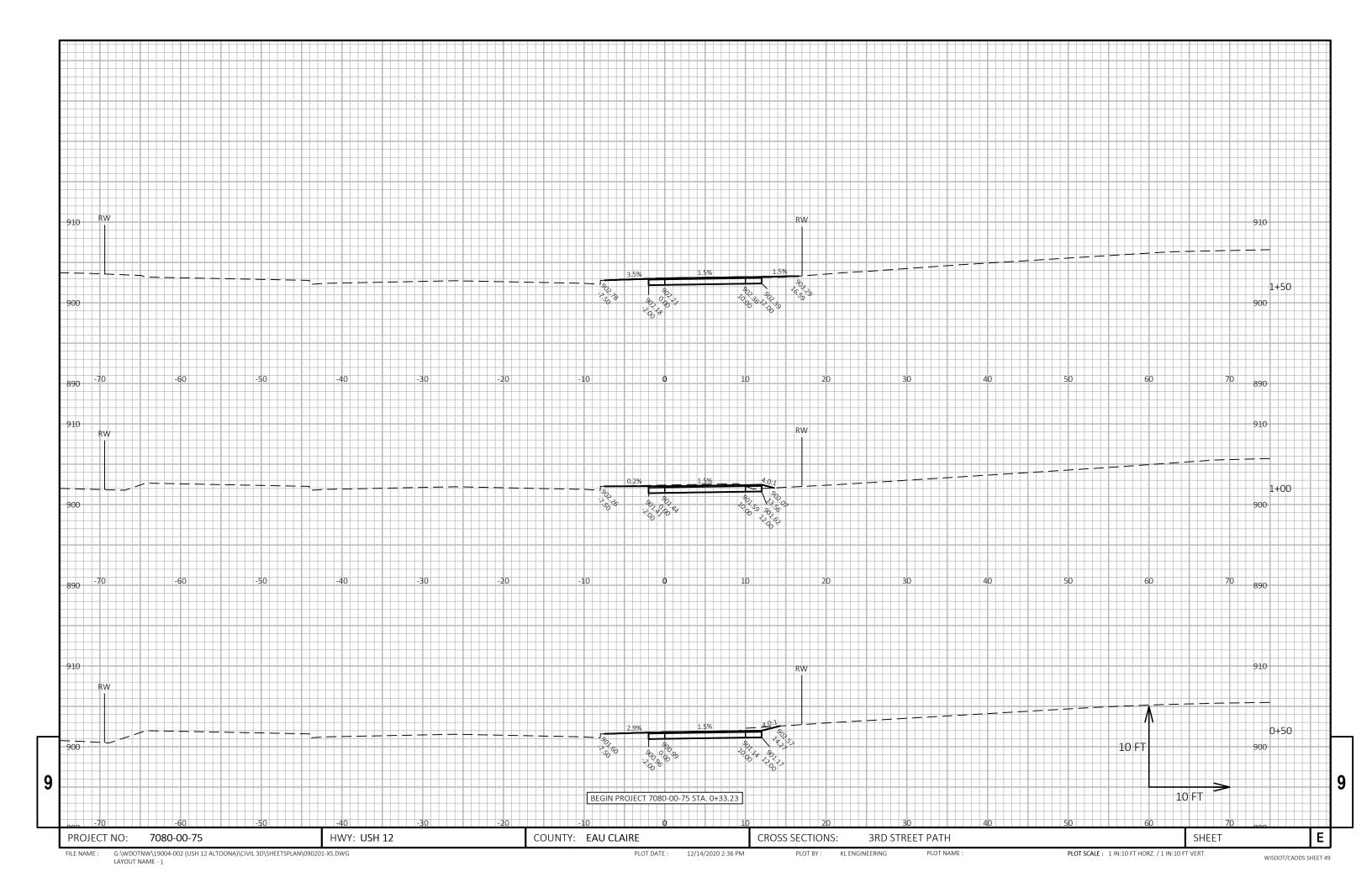
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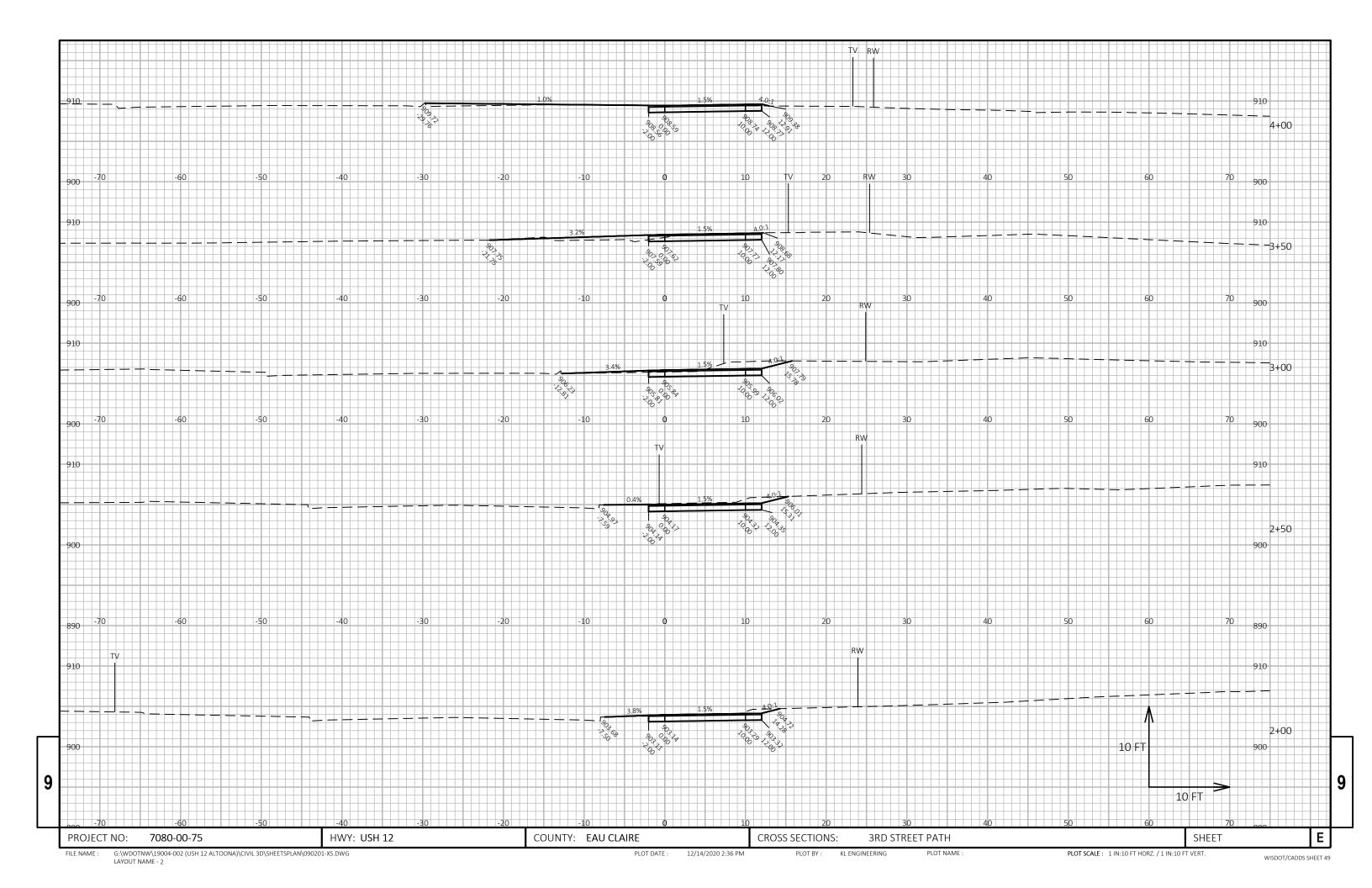
PLOT BY: KL ENGINEERING

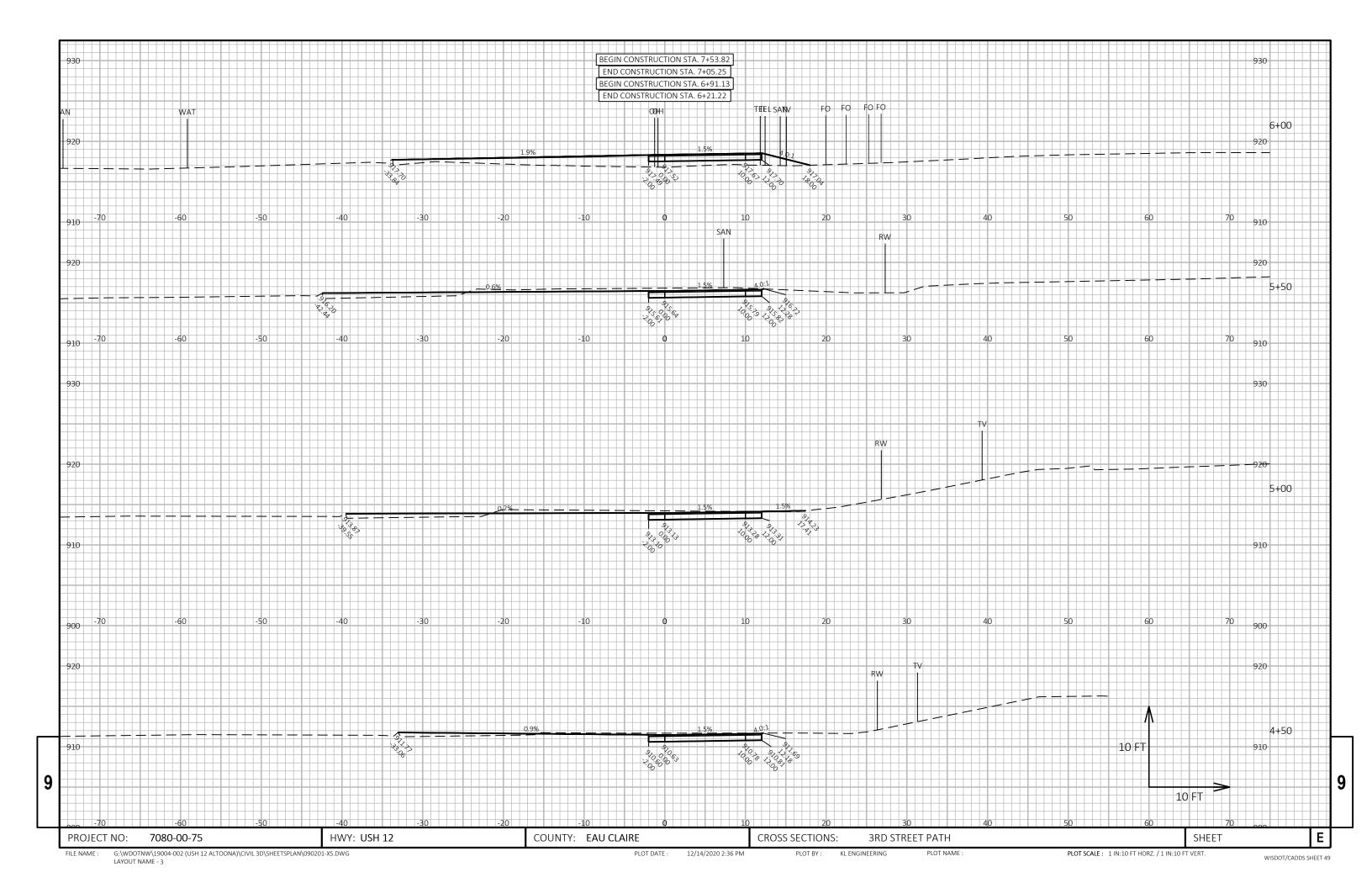
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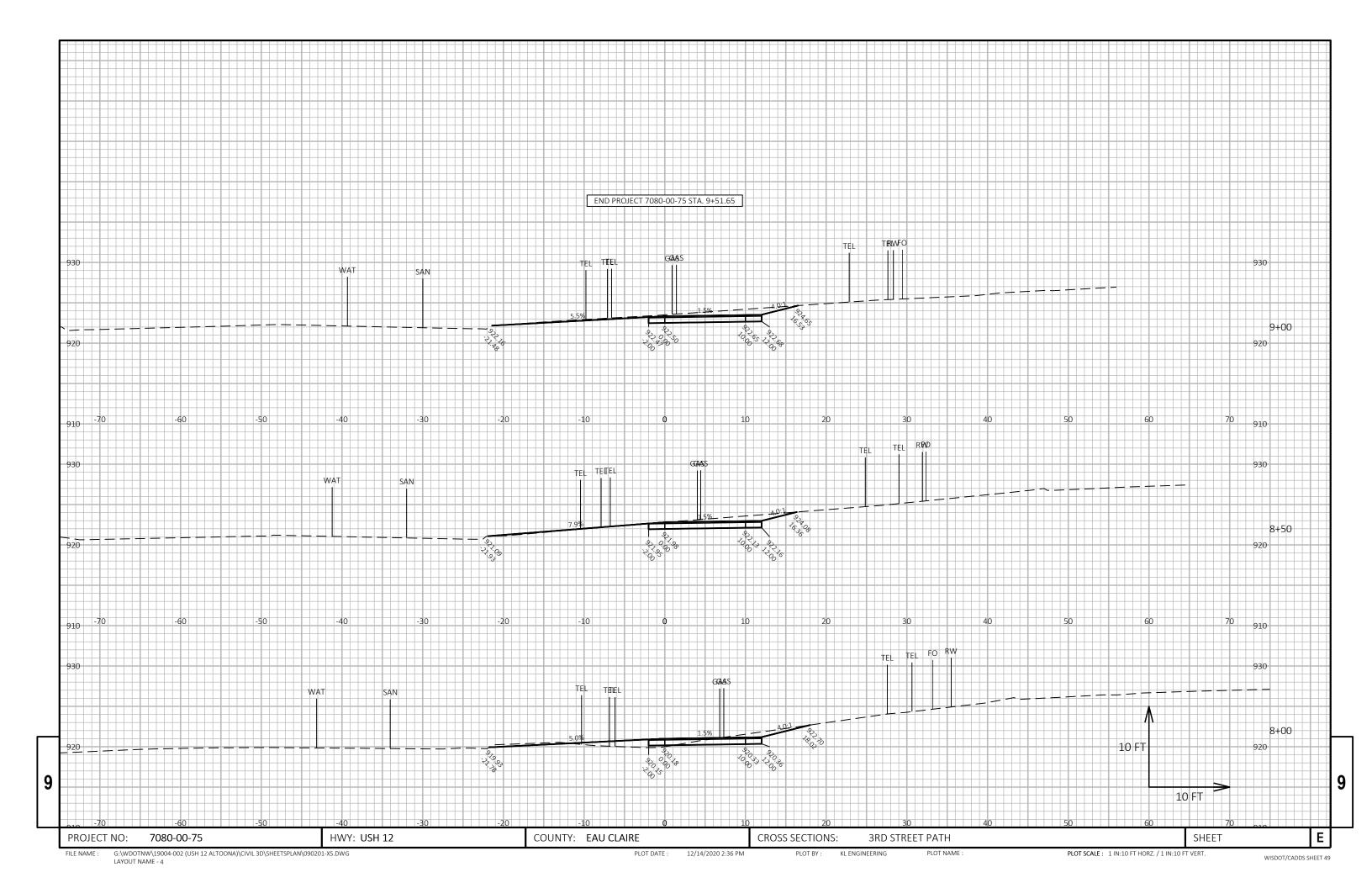
PLOT SCALE : Custom

WISDOT/CADDS SHEET 42

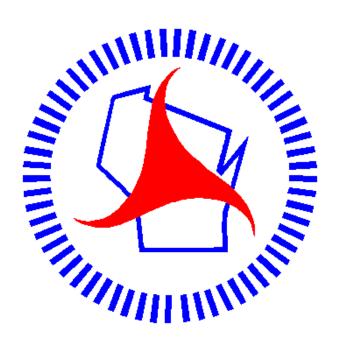








Notes



# Wisconsin Department of Transportation

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