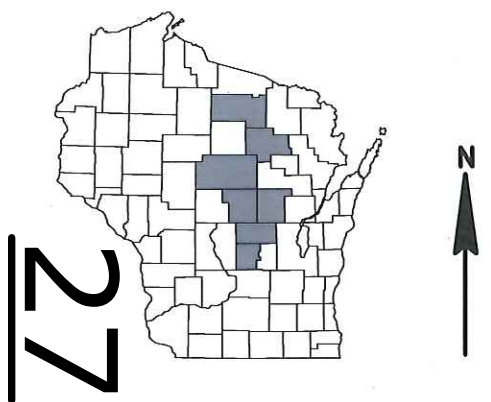


Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 68



DESIGN DESIGNATION

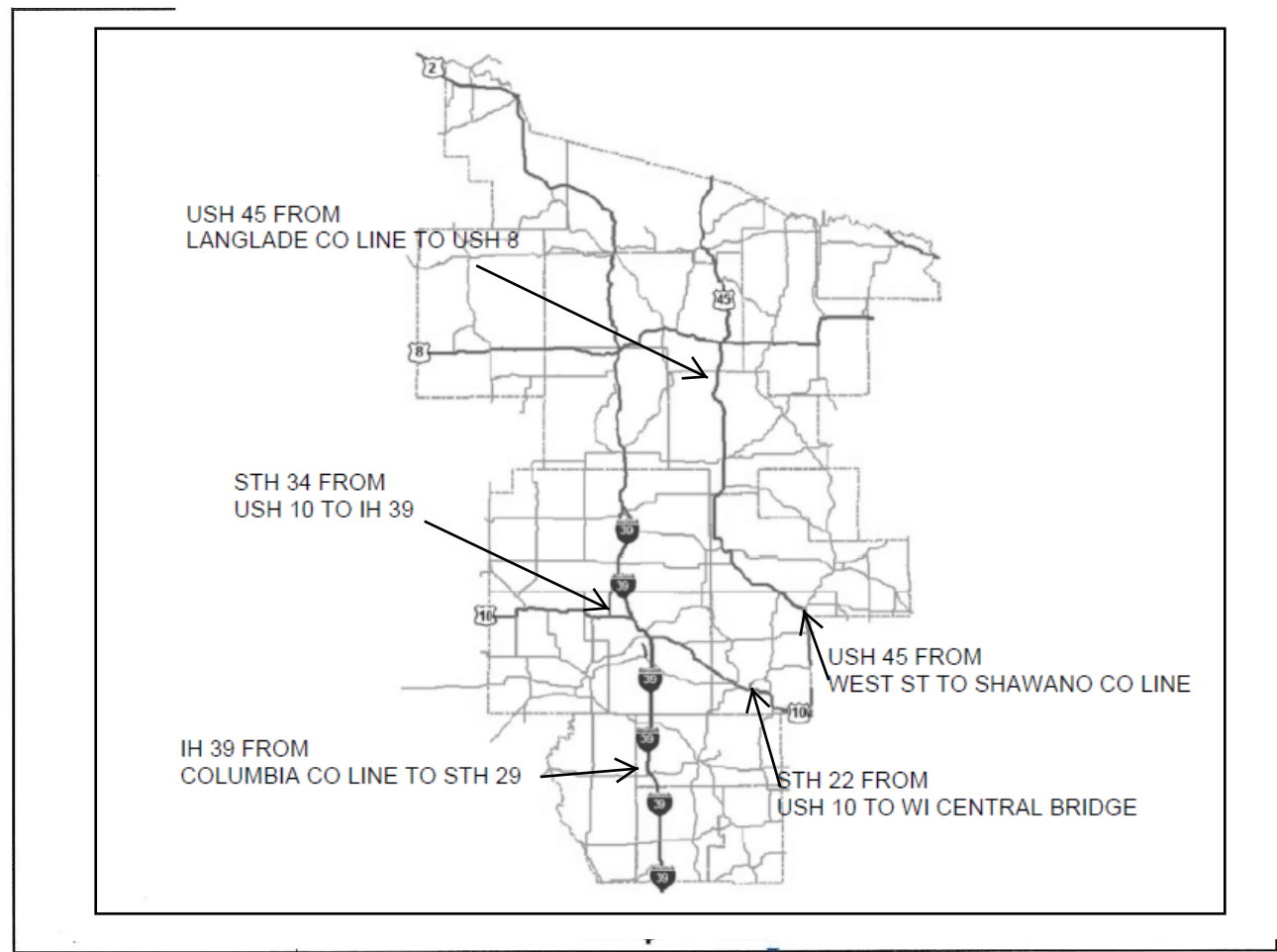
A.A.D.T. = VARIES
A.A.D.T. VARIES = VARIES
D.H.V. = VARIES
D.D. = VARIES
T. = VARIES
DESIGN SPEED = VARIES
ESALS = VARIES

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	
EXISTING RIGHT OF WAY		GRADE ELEVATION	
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE		ELECTRIC	
EXISTING CULVERT		FIBER OPTIC	
PROPOSED CULVERT (Box or Pipe)		GAS	
COMBUSTIBLE FLUIDS		SANITARY SEWER	
MARSH AREA		STORM SEWER	
WOODED OR SHRUB AREA		TELEPHONE	
		WATER	
		UTILITY PEDESTAL	
		POWER POLE	
		TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
NC REGION, EPOXY PAVEMENT MARKING
LOCATIONS ON STN PER ANNUAL PLAN
VAR HWY
NORTH CENTRAL REGION WIDE

STATE PROJECT NUMBER
1009-42-25



LAYOUT
SCALE 0 1 MILE
TOTAL NET LENGTH OF CENTERLINE =

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
PROJECT ID	WISC 2021093	1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor _____
Designer JEANNE SILVER
Project Manager JEANNE SILVER
Regional Examiner _____
Regional Supervisor MATT RAUCH

APPROVED FOR THE DEPARTMENT

DATE 11/5/2020 Matt Rauch (Signature)

E

GENERAL NOTES
THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS, COORDINATE CONSTRUCTON ACTIVIITES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITEIS IN THE AREA. NOT ALL UTILIITES ARE MEMBERS OF DIGGERS HOTLINE.



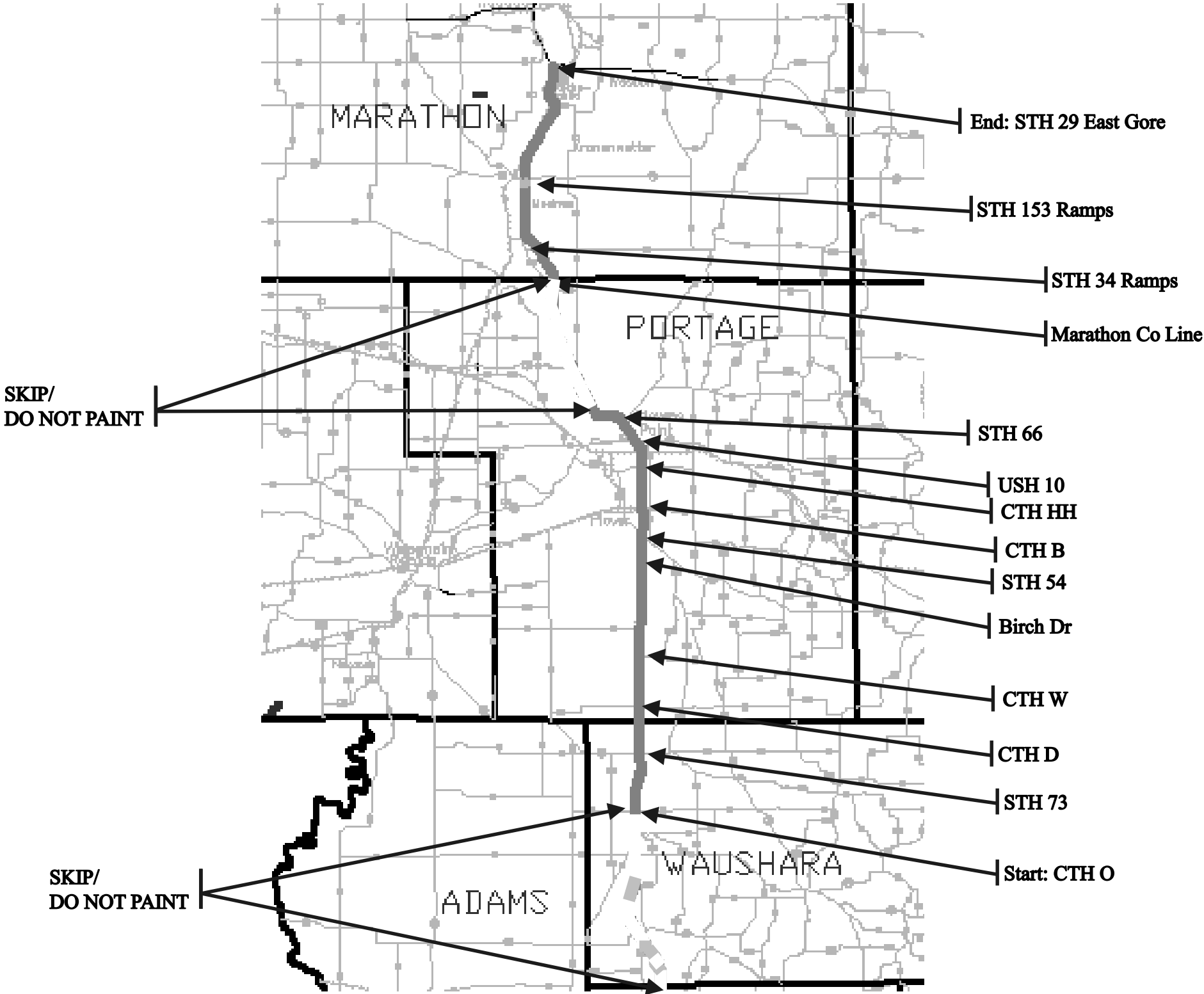
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608-246-5408

MARQUETTE, WAUSHARA, PORTAGE & MARATHON COUNTY

IH 39NB FROM COLUMBIA COUNTY LINE TO STH 29

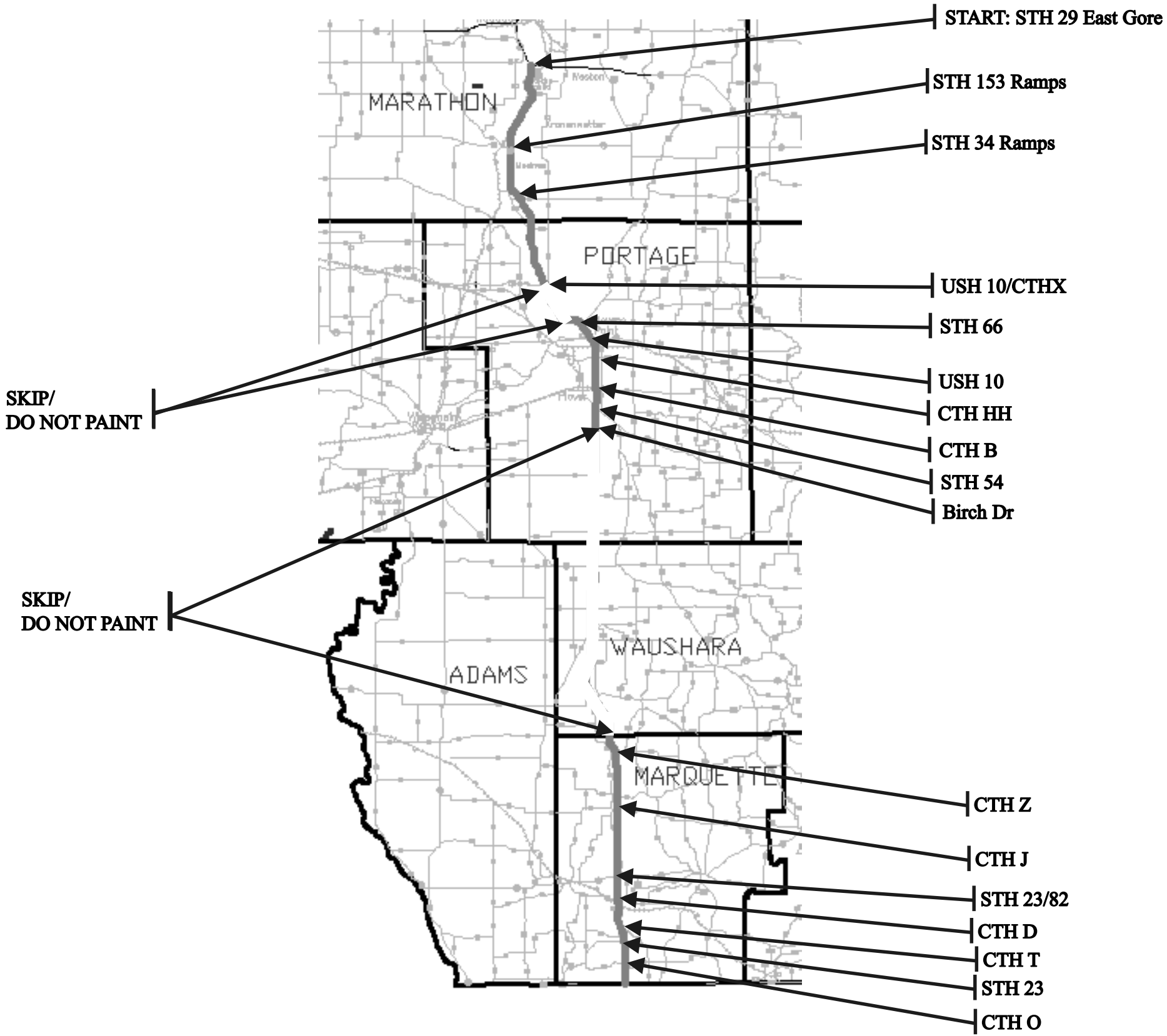


2

MARQUETTE, WAUSHARA, PORTAGE & MARATHON COUNTY

IH 39SB FROM COLUMBIA COUNTY LINE TO STH 29

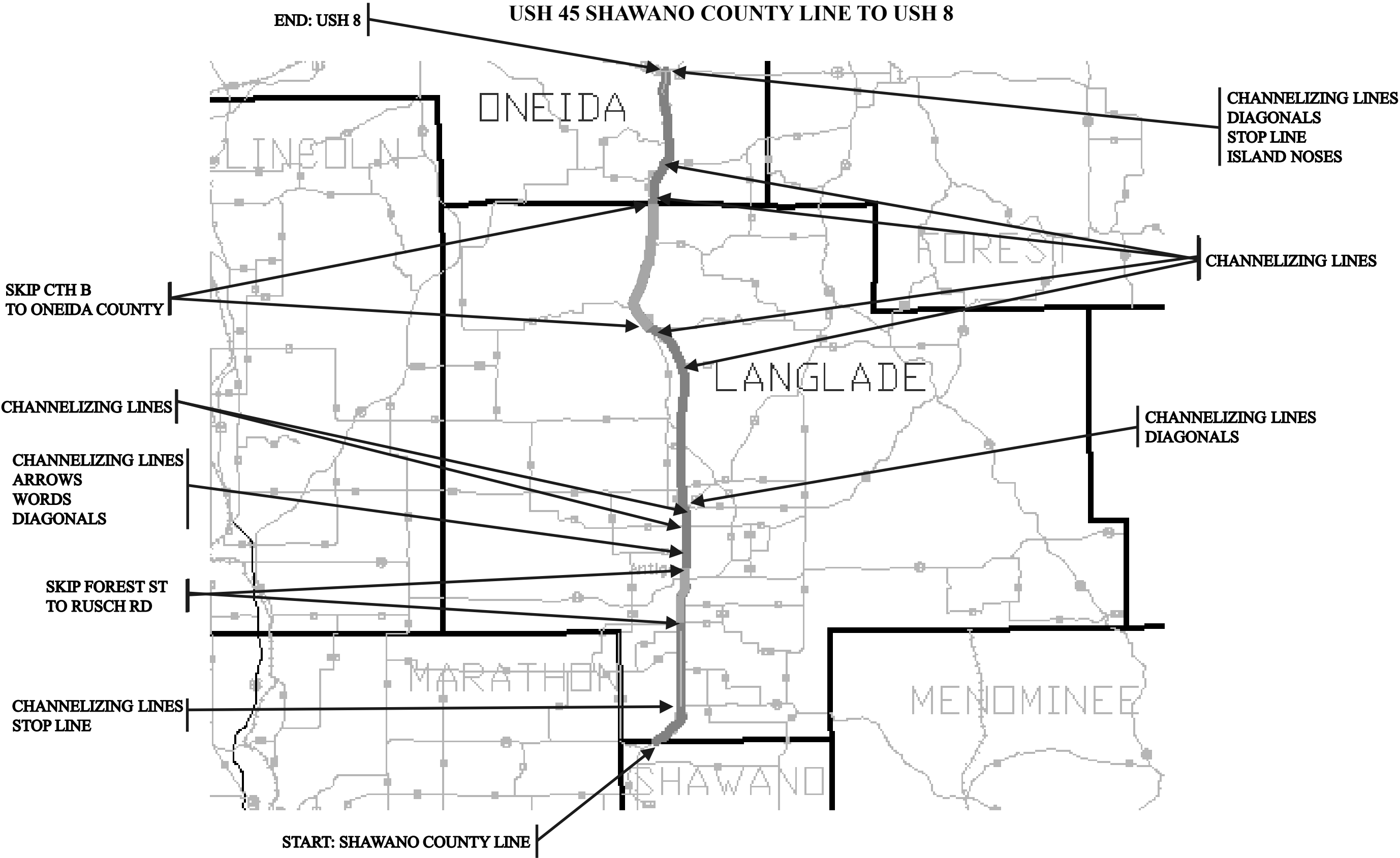
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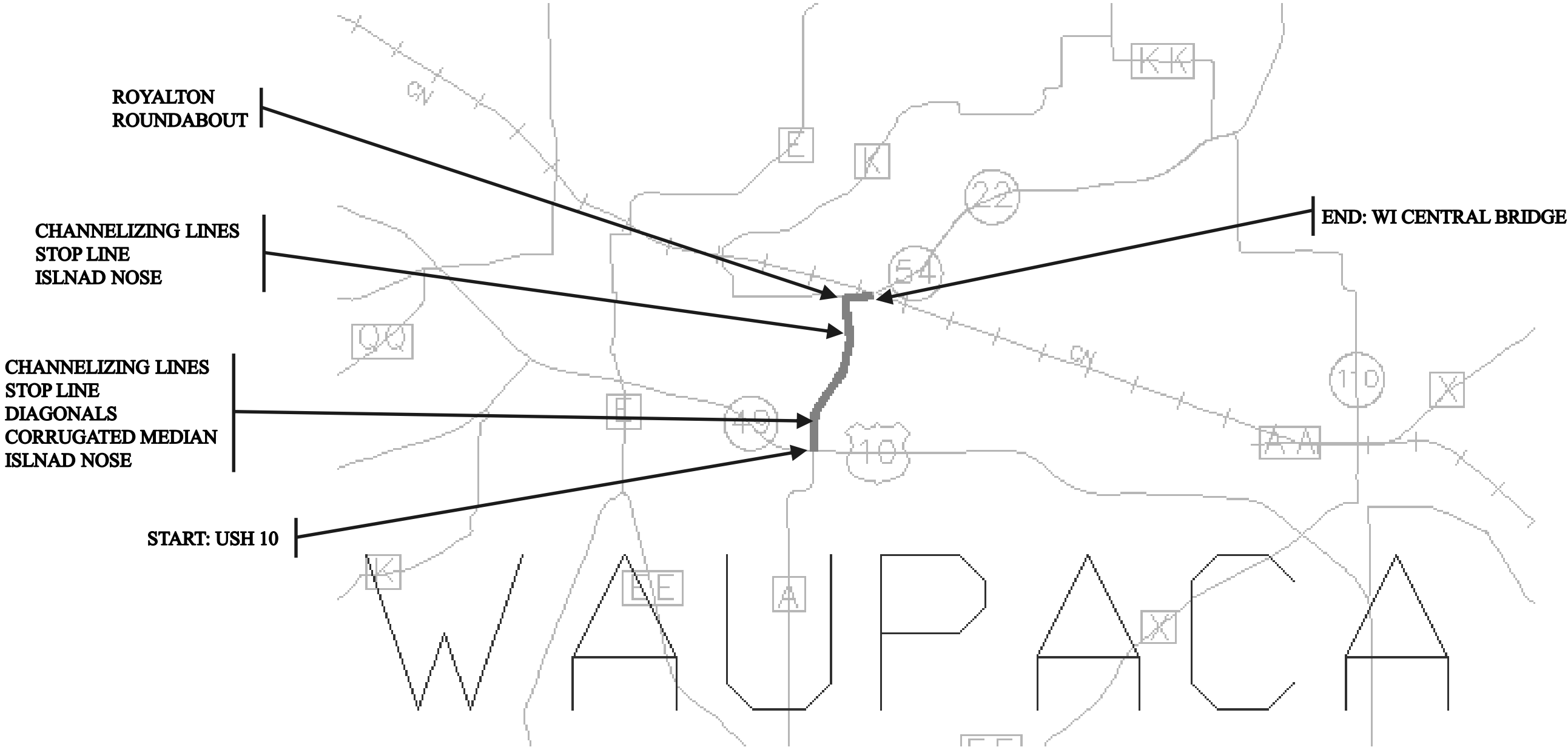
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LANGLADE/ONEIDA COUNTY
USH 45 SHAWANO COUNTY LINE TO USH 8



WAUPACA COUNTY
STH 22/54 FROM USH 10 TO CENTRAL RIVER RAILROAD

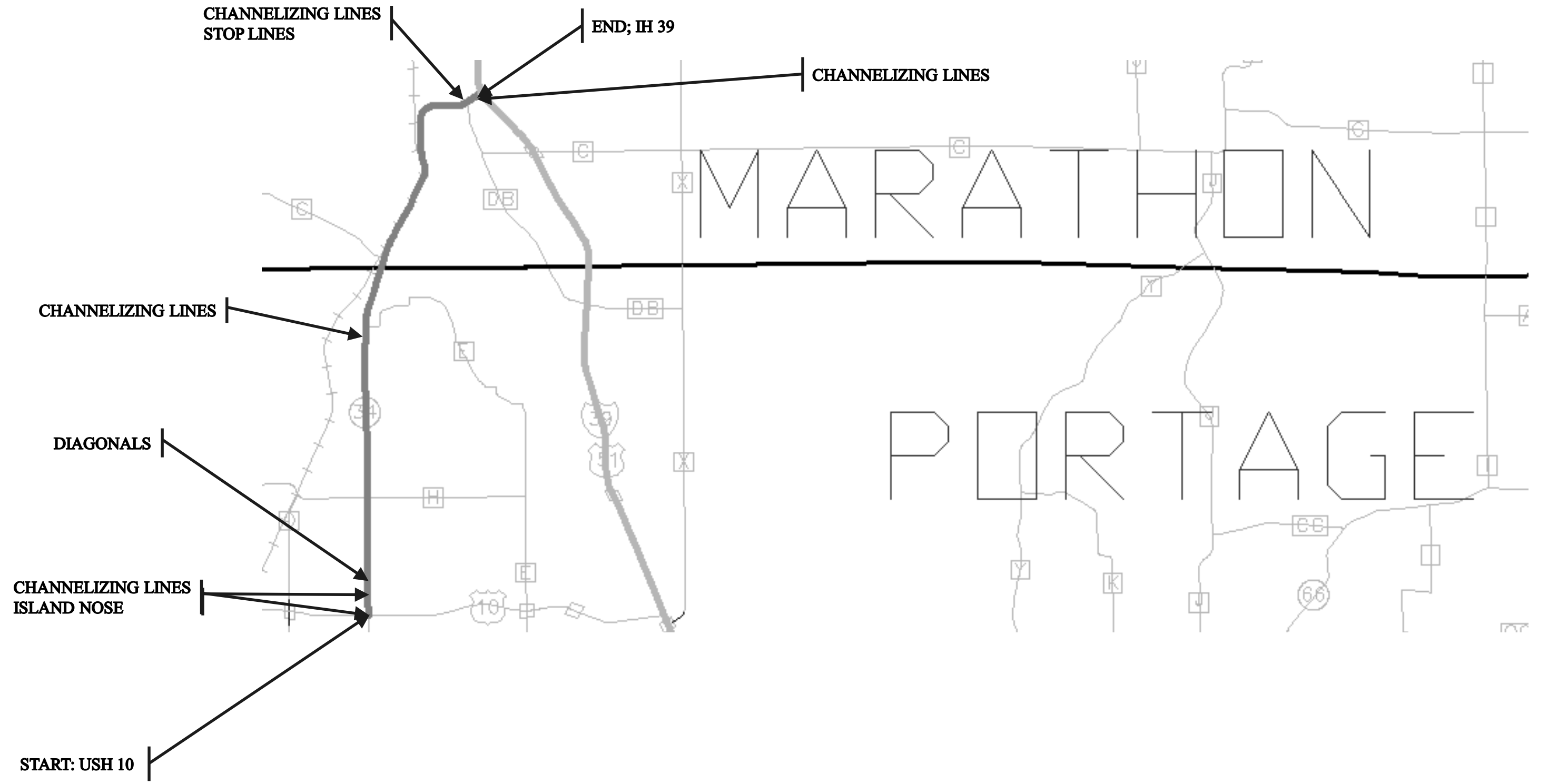


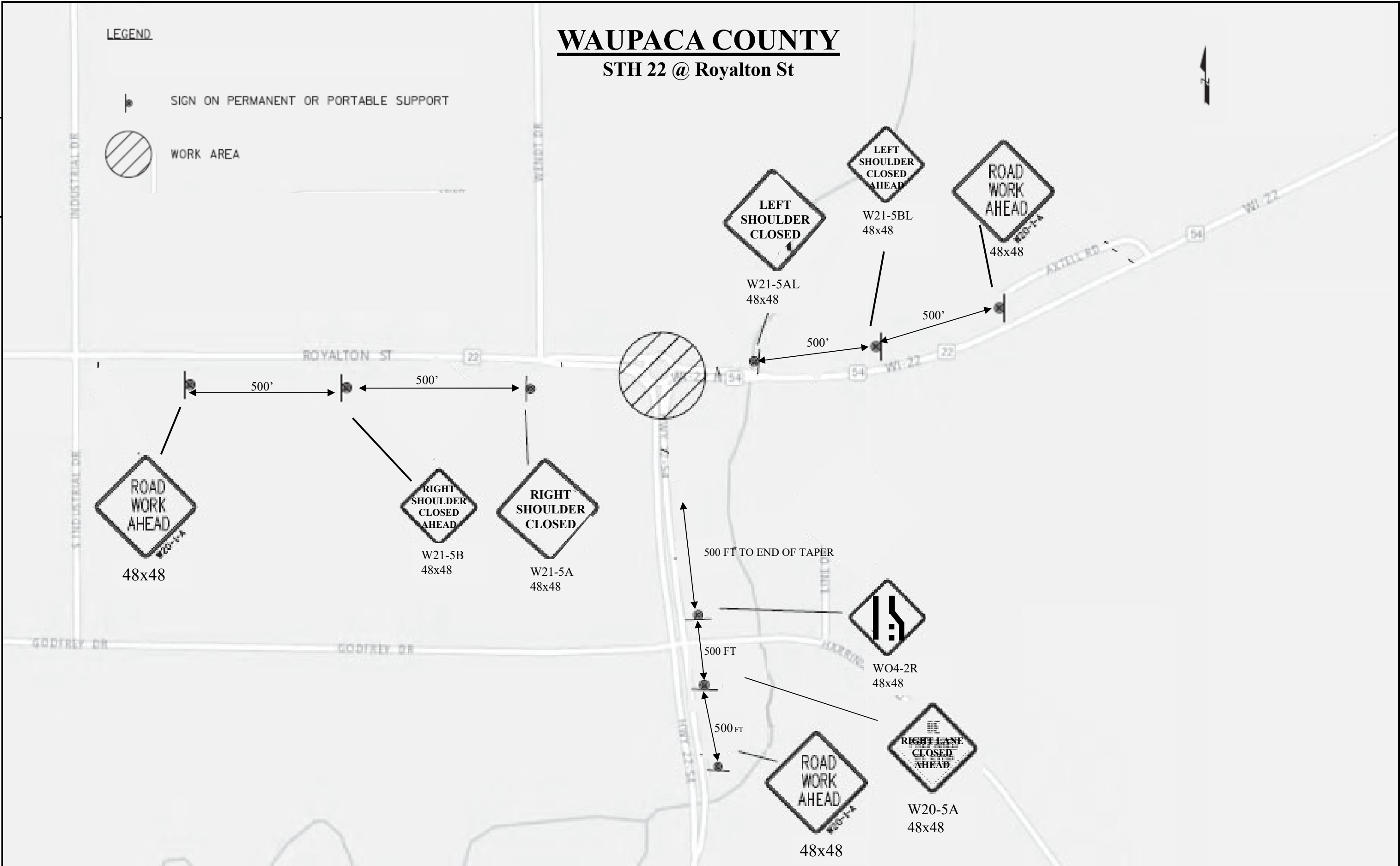
MARATHON PORTAGE COUNTY

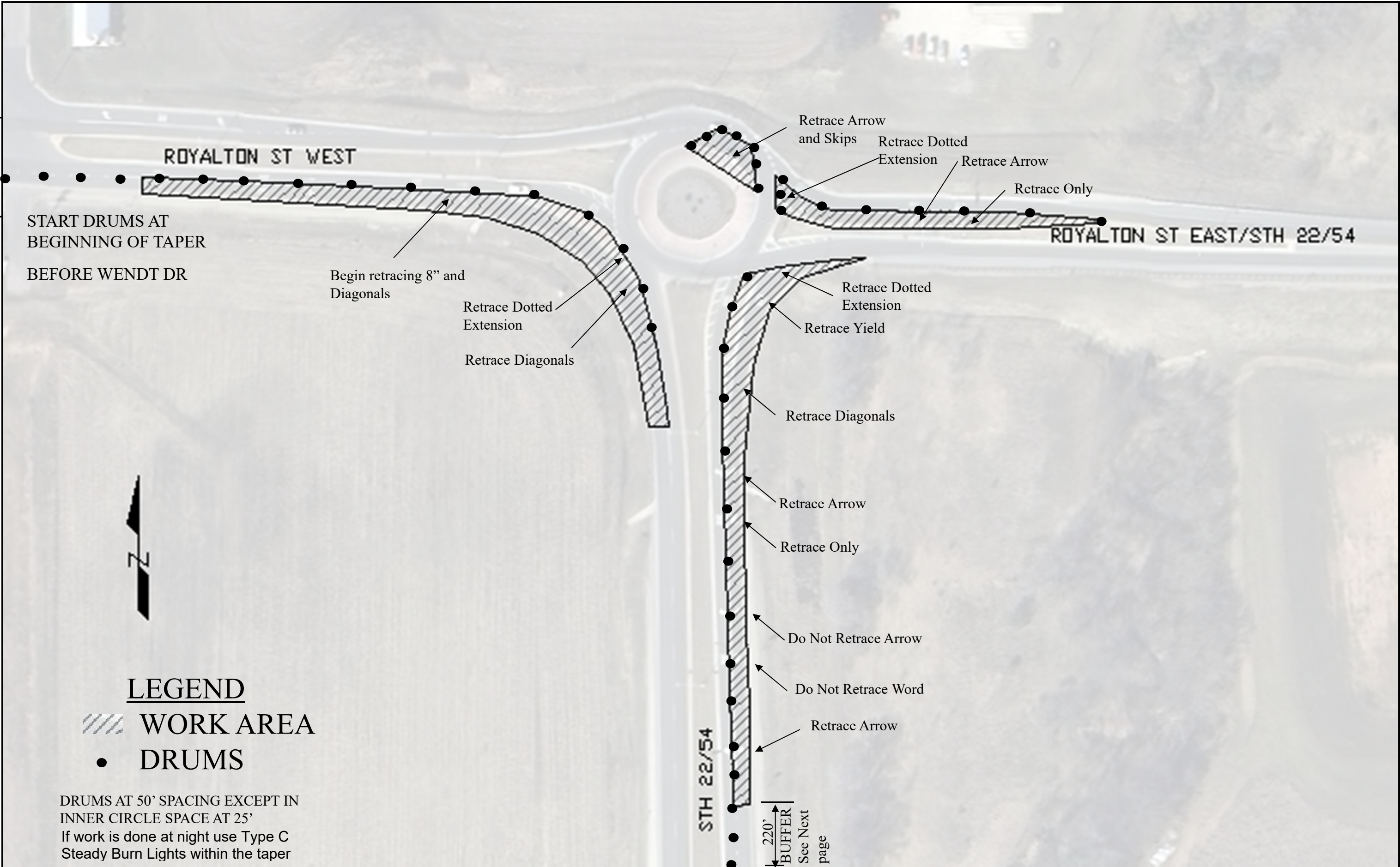
STH 34 USH 10 TO IH 39

2

2







WAUPACA COUNTY
STH 22 @ Royalton St



220' BUFFER

540' TAPER

ARROW
BOARD

LEGEND

 **WORK AREA**

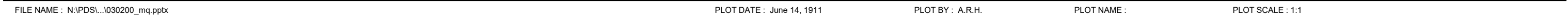
• **DRUMS**

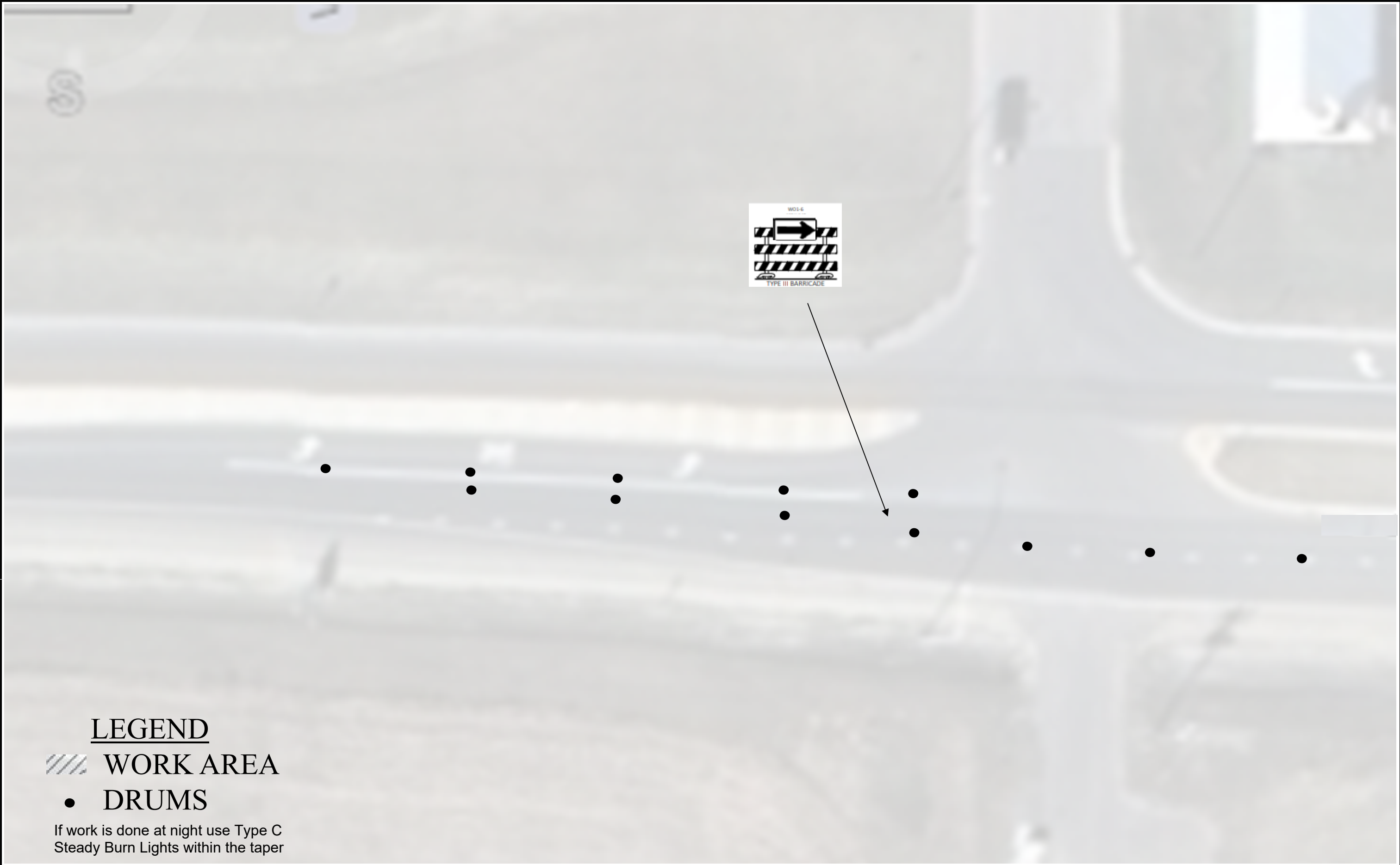
DRUMS AT 50' SPACING EXCEPT IN IN CIRCLE SPACE AT 25'
If work is done at night use Type C Steady Burn Lights within the taper

LEGEND

SIGN ON PERMANENT OR PORTABLE SUPPORT

WORK AREA





WAUPACA COUNTY

STH 22 @ Royalton St

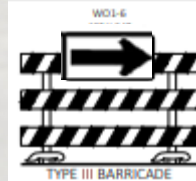


2

2

ROYALTON ST WEST

ROYALTON ST EAST/STH 22/54



Retrace Diagonals

Retrace Dotted Extension

Retrace Dotted Extension

Retrace Yield

Retrace Arrow

Retrace Only

200'



LEGEND

 WORK AREA

• DRUMS

DRUMS AT 50' SPACING EXCEPT IN IN CIRCLE SPACE AT 25'
If work is done at night use Type C Steady Burn Lights within the taper

Retrace Dotted Extension

Retrace Yield

Retrace Diagonals

Retrace Arrow

Retrace Only

Do Not Retrace Arrow

Do Not Retrace Word

Retrace Arrow

STH 22/54

220'

BUFFER ZONE
SEE NEXT PAGE

WAUPACA COUNTY
STH 22 @ Royalton St

2

2



220' BUFFER

540' TAPER

ARROW
BOARD

GODFREY DR

LEGEND

 **WORK AREA**

● **DRUMS**

DRUMS AT 50' SPACING EXCEPT IN IN CIRCLE SPACE AT 25'
If work is done at night use Type C Steady Burn Lights within the taper

Estimate Of Quantities

1009-42-25

Line	Item	Item Description	Unit	Total	Qty
0002	619.1000	Mobilization	EACH	1.000	1.000
0004	646.1020	Marking Line Epoxy 4-Inch	LF	1,899,685.000	1,899,685.000
0006	646.3020	Marking Line Epoxy 8-Inch	LF	19,563.000	19,563.000
0008	646.5020	Marking Arrow Epoxy	EACH	106.000	106.000
0010	646.5120	Marking Word Epoxy	EACH	33.000	33.000
0012	646.6120	Marking Stop Line Epoxy 18-Inch	LF	1,095.000	1,095.000
0014	646.6320	Marking Dotted Extension Epoxy 18-Inch	LF	180.000	180.000
0016	646.7120	Marking Diagonal Epoxy 12-Inch	LF	2,306.000	2,306.000
0018	646.8020	Marking Corrugated Median Epoxy	SF	275.000	275.000
0020	646.8220	Marking Island Nose Epoxy	EACH	51.000	51.000
0022	SPV.0105	Special 01. Traffic Control	LS	1.000	1.000

SUMMARY TABLE

NORTH CENTRAL REGIONWIDE EPOXY PAVEMENT MARKING

ROUTE	COUNTY	LOCATION	646.1020 EPOXY 4-IN		646.3020 EPOXY 8-IN	646.5020 ARROW EPOXY			646.5120 WORD EPOXY		646.6120 STOP LINE EPOXY 18-IN	646.6320 DOTTED EXTENSION EPOXY 18-IN	646.7120 DIAGONAL EPOXY 12-INCH		646.8020 CORRUGATED MEDIAN EPOXY	646.8220 ISLAND NOSE EPOXY
			YELLOW LF	WHITE LF	WHITE LF	TYPE 1 EACH	TYPE 2 EACH	TYPE 2R EACH	ONLY EACH	YIELD EACH	WHITE LF	WHITE LF	YELLOW LF	WHITE LF	YELLOW SF	YELLOW EACH
WHOLE PROJECT	REGIONWIDE															
I39 SB	MARATHON	STH 29 TO MARQUETTE/WAUSHARA CO LINE	315,289	334,405	1,498	---	6	---	3	---	341	---	---	---	---	---
I39NB	WAUSHARA	MARQUETTE CO LINE TO STH 29	253,494	266,651	1,984	---	2	---	1	---	423	---	---	---	---	---
USH 45	WAUPACA	WEST ST TO SHAWANO CO LINE	85,800	91,186	4,095	---	53	---	14	---	---	---	---	120	---	24
STH 34	PORTAGE/MARATHON	USH 10 TO IH 39	48,733	104,301	2,118	---	---	---	---	---	70	---	236	---	---	6
USH 45	SHAWANO/LANGLADE	SHAWANO/LANGLED CO LINE TO USH 8	161,160	186,124	5,428	---	37	---	7	---	162	---	985	468	---	11
STH 22	WAUPACA	USH 10 TO WI CENTRAL BRIDGE	25,670	26,872	4,440	2	3	3	4	4	99	180	257	240	275	10
SUB-TOTAL			890,146	1,009,539		2	101	3	29	4	1,095	180	1,478	828		
TOTAL			1,899,685		19,563	106			33		1,095	180	2,306		275	51

	619.1000 Mobilization EACH	SPV.0105.01 Traffic Control EACH
Project Wide	1	1

MARQUETTE, WAUSHARA, PORTAGE & MARATHON COUNTY
IH 39 FROM COLUMBIA COUNTY LINE TO STH 29

3

3

ROUTE: IH 39NB MARQUETTE CO LINE TO STH 29			646.1020 EPOXY 4-IN		646.3020 EPOXY 8-IN	646.5020 ARROW EPOXY TYPE 2 EACH	646.5120 WORD EPOXY ONLY EACH	646.6120 STOP LINE EPOXY 18-INCH WHITE LF	COMMENTS
COUNTY	START LOCATION	END LOCATION	YELLOW LF	WHITE LF	WHITE LF				
WAUSHARA	I-39 NB MARQUETTE/ WAUSHARA LINE	CTH O							PROJECT SKIP
WAUSHARA	CTH O	2ND ST	154,704	154,704					EDGELINES ONLY
WAUSHARA	ELK RD BRIDGE								PROJECT SKIP
PORTAGE	2ND ST	MARATHON CO LINE							PROJECT SKIP
PORTAGE/MARATHON	750' SOUTH OF MILE 172	1600' SOUTH OF MILE 175		3,750					LANE LINES ONLY
MARATHON	MARATHON CO LINE	STH 29 EAST GORE	80,256	80,256					EDGELINES ONLY
WAUSHARA	STH 73 ON AND OFF RAMPS	RAMPS	1,981	2,858	73			51	
PORTAGE	CTH D ON AND OFF RAMPS	RAMPS	1,790	3,141	148			49	
PORTAGE	CTH W ON AND OFF RAMPS	RAMPS	1,833	3,127	164			48	
PORTAGE	STH 54/BUS 51 ON AND OFF RAMPS	RAMPS	1,707	2,682	466			50	
PORTAGE	PLOVER RD ON AND OFF RAMPS	RAMPS	1,286	2,113	99			38	
PORTAGE	CTH HH ON AND OFF RAMPS	RAMPS	1,691	2,806	581	2	1	21	
PORTAGE	USH 10/STH 66 SKIP RAMPS								PROJECT SKIP
PORTAGE	STH 66/STANLEY ST RAMPS								PROJECT SKIP
PORTAGE	USH 51 RAMPS								PROJECT SKIP
MARATHON	I-39 NB & STH 34	RAMPS	1,979	2,828	102			44	
MARATHON	I-39 NB & STH 153	RAMPS	1,622	2,453	114			38	
MARATHON	I-39 NB & MAPLE RIDGE RD	RAMPS	1,994	2,738	134			44	
MARATHON	I-39 NB & BUS 51 ROTHCHILD	RAMPS	2,651	3,195	103			40	
MARATHON	STH 29 EAST GORE								
			253,494	266,651	1,984	2	1	423	
			520,145		1,984	2	1	423	

MARQUETTE, WAUSHARA, PORTAGE & MARATHON COUNTY
IH 39 FROM STH 29 TO COLUMBIA COUNTY LINE

ROUTE:I39 SB
STH 29 TO MARQUETTE/WAUSHARA CO LINE

COUNTY	START LOCATION	END LOCATION	646.1020 EPOXY 4-IN		646.3020 EPOXY 8-IN WHITE LF	646.5020 ARROW EPOXY TYPE 2 EACH	646.5120 WORD EPOXY ONLY EACH	646.6120 STOP LINE EPOXY 18-INCH WHITE LF	COMMENTS
			YELLOW LF	WHITE LF					
MARATHON	STH 29 MM 187	CTH/X 3807' SOUTH OF MM 167	109,407	109,407					EDGELINES ONLY
MARTHAN	CTH/X 3807' SOUTH OF MM 167	2ND ST MM 162							PROJECT SKIP
PORTAGE	2ND ST MM 162	BIRCH DR/3500' SOUTH OF MM 151	61,580	61,580					EDGELINES ONLY
PORTAGE	BIRCH DR/3500' SOUTH OF MM 151	MARQUETTE/WAUSHARA CO LINE MM 120							PROJECT SKIP
MARQUETTE	MARQUETTE/WAUSHARA CO LINE MM 120	COLUMBIA COUNTY LINE MM 96.3	125,136	125,136					
MARATHON/PORTAGE	900' SOUTH OF MM 175	BUS 51 OFF RAMP (MM 167)		10,350					LANE LINES ONLY
MARATHON	I-39 SB & BUS 51 ROTHCHILD	RAMPS	1,752	2,093				14	DO NOT REPLACE CHEVRONS
MARATHON	I-39 SB & MAPLE RIDGE RD	RAMPS	1,928	2,804	100			49	DO NOT REPLACE CHEVRONS
MARATHON	I-39 SB & STH 153	RAMPS	1,881	2,929	320			50	DO NOT REPLACE CHEVRONS
MARATHON	I-39 SB & STH 34	RAMPS	2,441	3,519	106			44	DO NOT REPLACE CHEVRONS
PORTAGE	I-39 SB & CTH DB	RAMPS	2,240	2,997	73			48	DO NOT REPLACE CHEVRONS
PORTAGE	CTH X	RAMPS	2,766	4,420					
PORTAGE	USH 10/STH 66 SKIP RAMPS								PROJECT SKIP
PORTAGE	STH 66/STANLEY ST RAMPS								PROJECT SKIP
PORTAGE	USH 51 RAMPS								PROJECT SKIP
PORTAGE	CTH HH	RAMPS	2,939	4,075	642	6	3	49	
PORTAGE	CTH B/PLOVER RD	RAMPS	1,350	2,284	107			36	
PORTAGE	STH 54	RAMPS	1,869	2,811	150			51	
SUBTOTAL			315,289	334,405					
TOTAL				649,694	1,498	6	3	341	

MILE MARKING 174.8 TO 165.7 MOVE THE YELLOW EDGELINE 6” IN ON TOP OF THE CONCRETE.
MILE MARKING 174.8 TO 165.7 RETRACE THE INNER WHITE EDGELINE.

WAUPACA COUNTY
USH 45 FROM WEST ST TO SHAWANO COUNTY LINE

ROUTE:USH 45 WEST ST TO SHAWANO CO LINE			646.1020 EPOXY 4-INCH		646.3020 EPOXY 8-IN	646.5020 ARROW EPOXY	646.5120 WORD EPOXY	646.7120 DIAGONAL EPOXY 12-INCH	646.8220 ISLAND NOSE EPOXY	COMMENTS
COUNTY	START LOCATION	END LOCATION	YELLOW LF	WHITE LF	YELLOW LF	TYPE 2 EACH	ONLY EACH	WHITE LF	YELLOW EACH	
WAUPACA	USH 45 NB WEST ST (CLINTONVILLE)	SHAWANO CO LINE	85,800	91,186		28				DO NOT REPLACE FIRST ARROW
WAUPACA	USH 45 @ WEST ST				198	1			2	
WAUPACA	USH 45 @ CTH C				324	2			2	
WAUPACA	USH 51 @ HILLTOP RD				175	2	2		2	
WAUPACA	USH 45 @ LAKESHORE RD				2,230	4	2	120	2	
WAUPACA	USH 45 @ ALBERTS RD				99	1	1		2	
WAUPACA	USH 45@ BUCKBEE RD				100	2	1		2	
WAUPACA	USH 45 @ KNITT RD				238	4	2		2	
WAUPACA	USH 45@ CTH S				425	5	3		2	
WAUPACA	USH 45 @ WAYSIDE				---				4	
WAUPACA	USH 45 @ N 7TH ST				202	2	2		2	
WAUPACA	USH 45 @ N 5TH ST				104	2	1		2	
SUBTOTAL			85,800	91,186						
TOTAL			176,986		4,095	53	14	120	24	

WAUPACA COUNTY
STH 22/54 FROM USH 10 TO CENTRAL RIVER RAILROAD

ROUTE:STH 22 USH 10 TO WI CENTRAL BRIDGE			646.1020 EPOXY 4-IN		646.3020 EPOXY 8-IN	646.5020 ARROW EPOXY			646.5120 WORD EPOXY		646.6120 STOP LINE EPOXY 18-IN	646.6320 DOTTED EXTENSION EPOXY 18-IN	646.7120 DIAGONAL EPOXY 12-INCH		646.8020 CORRUGATED MEDIAN EPOXY	646.8220 ISLAND NOSE EPOXY	COMMENTS
COUNTY	START LOCATION	END LOCATION	YELLOW LF	WHITE LF	WHITE LF	TYPE 1 EACH	TYPE 2 EACH	TYPE 2R EACH	ONLY EACH	YIELD EACH	WHITE LF	WHITE LF	YELLOW LF	WHITE LF	YELLOW SF	YELLOW EACH	
WAUPACA	USH 10	WI CENTRAL BRIDGE	25,670	26,872	---												MAINLINE ONLY. DO NOT PAINT RAMPS
WAUPACA	STH 22 @ USH 10 RAMPS				1,328											4	
WAUPACA	STH 22 @ RUNWAY DR/APPLE TREE LN				504						54		177		275	2	
WAUPACA	STH 22 @ HARRINGTON RD				408						45					2	SKIP SECOND SET OF ARROWS/WORDS
WAUPACA	STH 22 @ ROYALTON ST.				2,200	2	3	3	4	4		180	80	240		2	
SUBTOTAL			25,670	26,872		2	3	3	4	4			257	240			
TOTAL			52,542		4,440		8		8		99	180	497		275	10	

LANGLADE/ONEIDA COUNTY
USH 45 SHAWANO COUNTY LINE TO USH 8

ROUTE: USH 45 SHAWANO/LANGLADE CO LINE TO USH 8			646.1020 EPOXY 4-IN		646.3020 EPOXY 8-IN	646.5020 ARROW EPOXY	646.5120 WORD EPOXY	646.6120 STOP LINE EPOXY 18-INCH	646.7120 DIAGONAL EPOXY 12-INCH		646.8220 ISLAND NOSE EPOXY	COMMENTS
COUNTY	START LOCATION	END LOCATION	YELLOW LF	WHITE LF	WHITE LF	TYPE 2 EACH	ONLY EACH	WHITE LF	YELLOW LF	WHITE LF	YELLOW EACH	
LANGLADE	SHAWANO/LANGLADE CO LINE	CTH B	117,098	121,192								PROJECTS CONNECTING HIGHWAY
LANGLADE	CTH B	ONEIDA CO LINE	---	---								
LANGLADE	ANTIGO CTH Y/FOREST ST	SOUTH OF RUSCH RD	---	---								
ONEIDA	ONEIDA CO LINE	USH 8	44,062	64,932								DO NOT REPLACE FIRST WORD
SHAWANO	USH 45 @ GAS STATION/BASSWOOD LN				339	1	1		191		3	
SHAWANO	USH 45 @ HO CHUNK				825	4	2		159	188		
SHAWANO	USH 45 @ CTH O/CEDAR ST				200							
SHAWANO	USH 45 @ CTH N							60				
SHAWANO	USH 45 @ CTH Z				390							
SHAWANO	USH 45 @ STH 52				200			22				
LANGLADE	USH 45 @ BEAR LAKE/STH 47				185			15				
LANGLADE	USH 45 @ RUSCH TO CTH N				550	32	4		221			
LANGLADE	USH 45 @ CTH I				860							
LANGLADE	USH 45 @ CTH A				840							
LANGLADE	USH 45 @ CTH B				250				360			
LANGLADE	USH 45 @ CTH J				280							
LANGLADE	USH 45 @ CTH B				115							
ONEIDA	USH 45 @ CTH G				90							
ONEIDA	USH 45 @ CTH B											
ONEIDA	USH 45 @ CTH Q				93							
ONEIDA	USH 45 @ USH 8				211			65	54	280	8	
SUBTOTAL			161,160	186,124					985	468		
TOTAL			347,284		5,428	37	7	162	1,453		11	

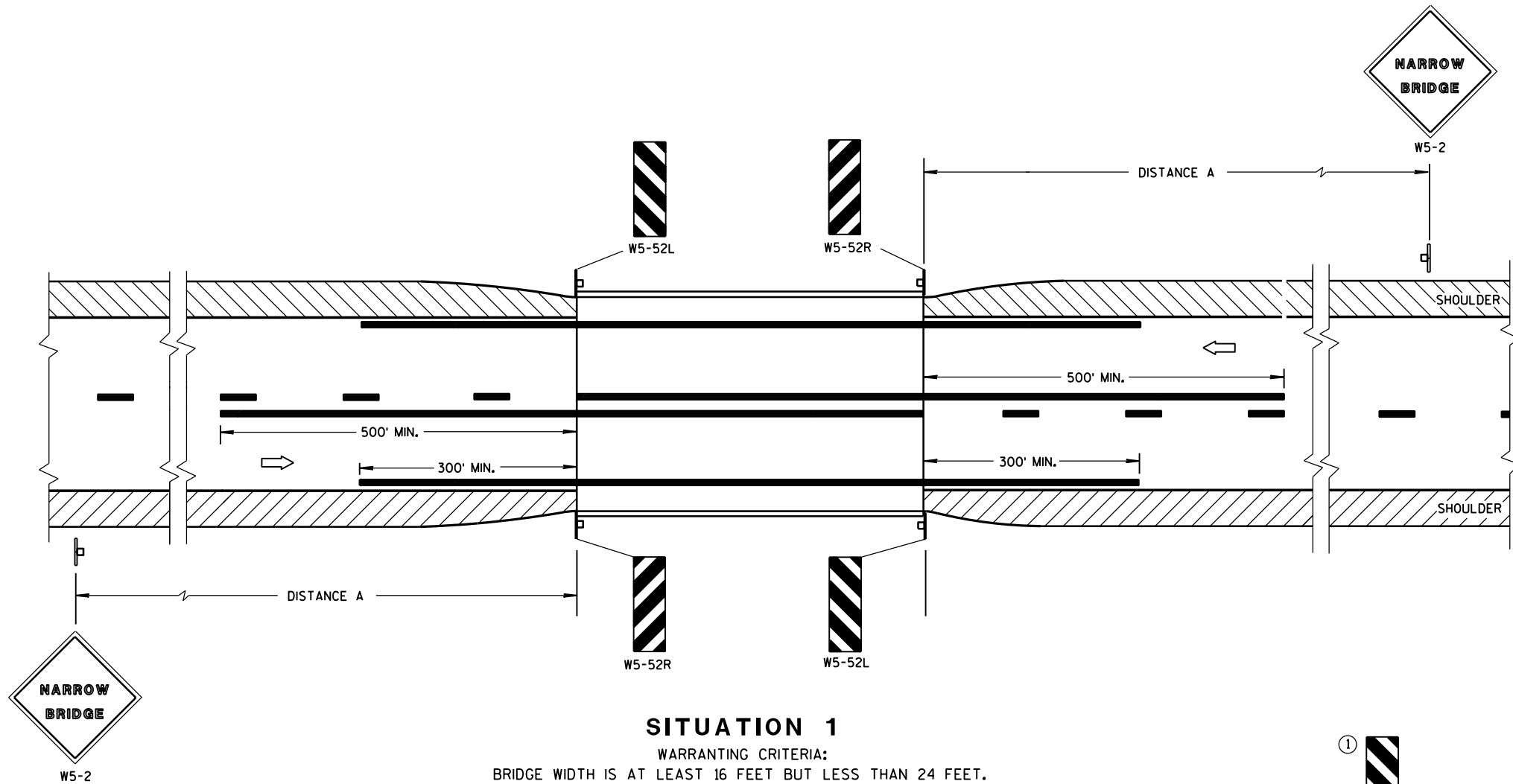
MARATHON PORTAGE COUNTY
STH 34 USH 10 TO IH 39

ROUTE: STH 34
USH 10 TO IH 39

COUNTY	START LOCATION	END LOCATION	646.1020 EPOXY 4-IN YELLOW LF	WHITE LF	646.3020 EPOXY 8-IN WHITE LF	646.6120 STOP LINE EPOXY 18-INCH WHITE LF	646.7120 DIAGONAL EPOXY 12-INCH YELLOW LF	646.8220 ISLAND NOSE EPOXY YELLOW EACH	COMMENTS
PORTAGE/MARATHON	USH 10EB OFF RAMP	I-39SB ON RAMP	48,733	104,301					
PORTAGE	STH 34 @ USH 10				672			4	
PORTAGE	STH 34 @ OAK HILL RD				780			2	
PORTAGE	STH 34 @ ELM LN						236		
PORTAGE	STH 34 @ KEES LN				106				
MARATHON	STH 34 @ CTH DB				385	70			
MARATHON	STH 34 @ IH 39 SB ON RAMP				175				
SUBTOTAL			48,733	104,301					
TOTAL			153,034		2,118	70	236	6	

Standard Detail Drawing List

15C06-09	SIGNING & MARKING FOR TWO LANE BRIDGES
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C07-15D	ROUNDAABOUT ARROWS
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C08-20B	PAVEMENT MARKING (TURN LANES)
15C08-20C	PAVEMENT MARKING (TURN LANES)
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C18-04	MEDIAN ISLAND MARKING
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C19-06B	MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY
15C19-06C	MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY
15C21-10	SIGNING AND MARKING FOR TWO LANE TO FOUR LANE DIVIDED TRANSITIONS
15C27-03A	DOUBLE ARROW WARNING SIGN PLACEMENT
15C27-03B	PAVEMENT MARKING (ISLANDS)
15C31-03A	PAVEMENT MARKING (RAMPS AND GORES)
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15C35-04B	PAVEMENT MARKING AND SIGNING (CLIMBING LANE & PASSING LANE)
15C35-04C	PAVEMENT MARKING AND SIGNING (CLIMBING LANE & PASSING LANE)
15D12-08A	TRAFFIC CONTROL, LANE CLOSURE
15D12-08B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION
15D20-04	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D20-05A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY
15D27-03	TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

GENERAL NOTES

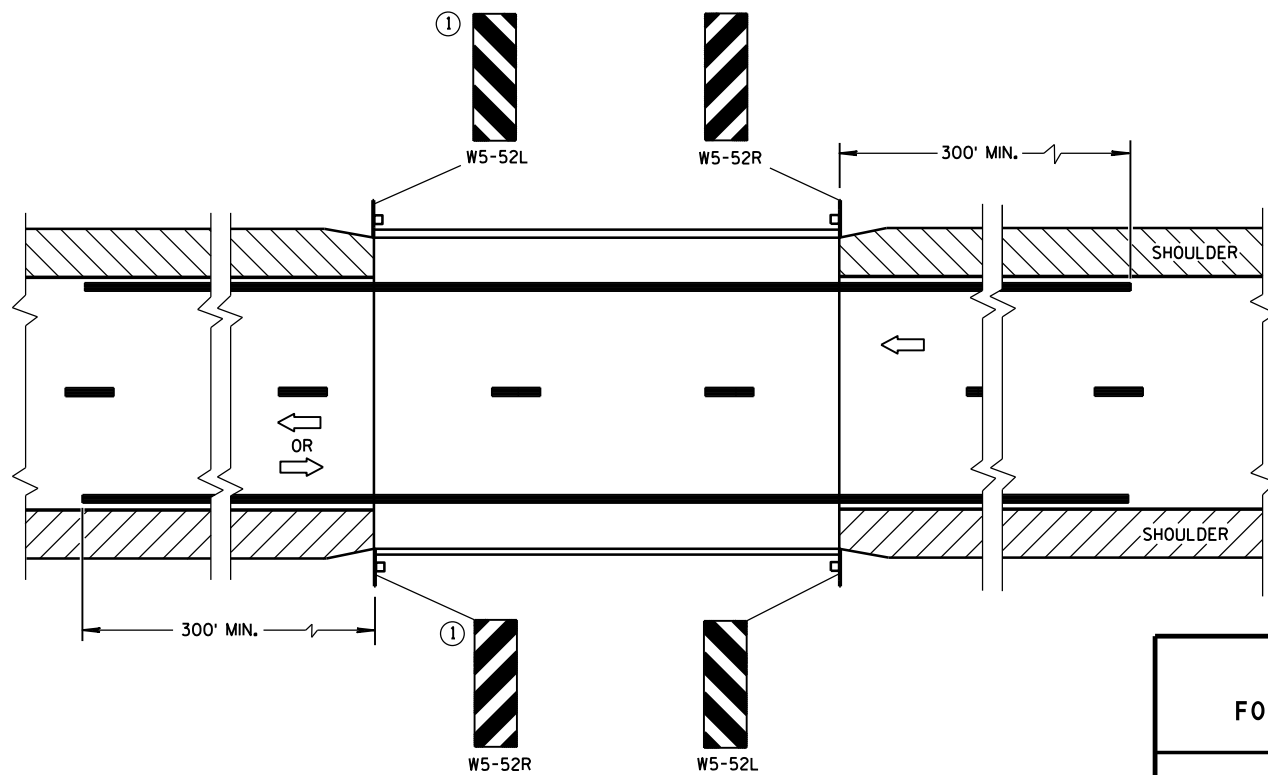
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

① OMIT ON ONE-WAY TRAVELLED WAYS.

➡ DIRECTION OF TRAFFIC



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

SIGNING & MARKING FOR TWO LANE BRIDGES

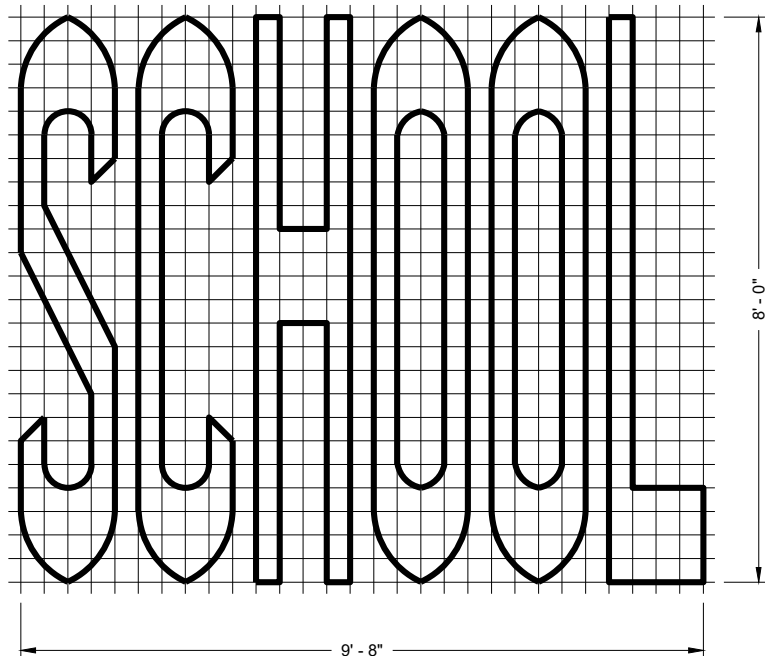
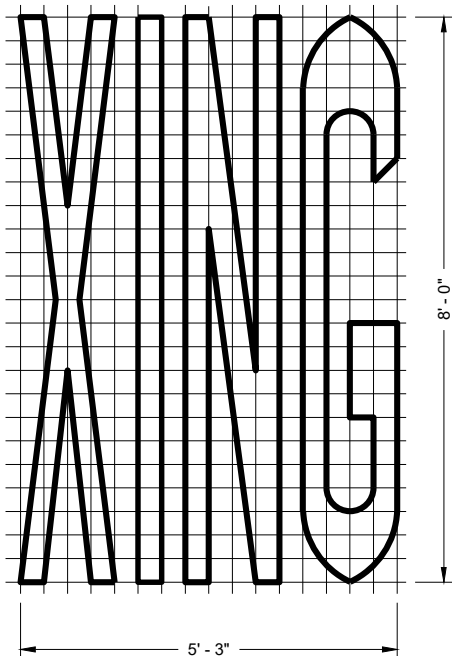
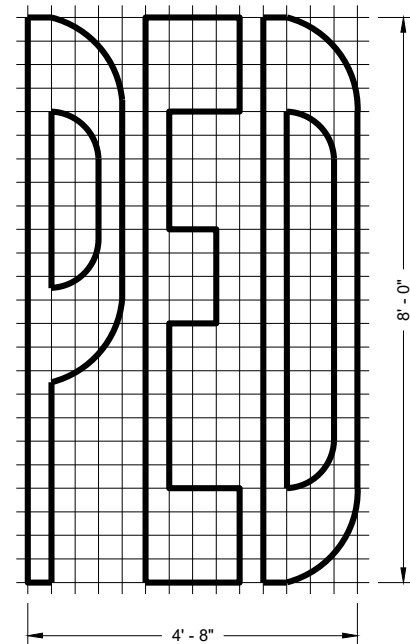
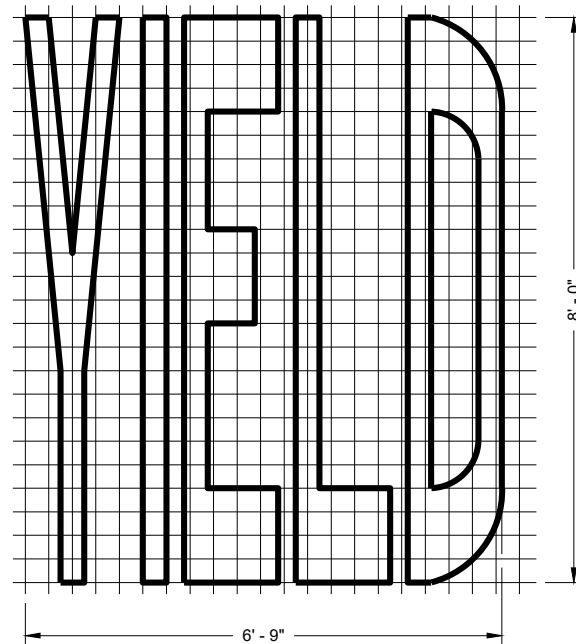
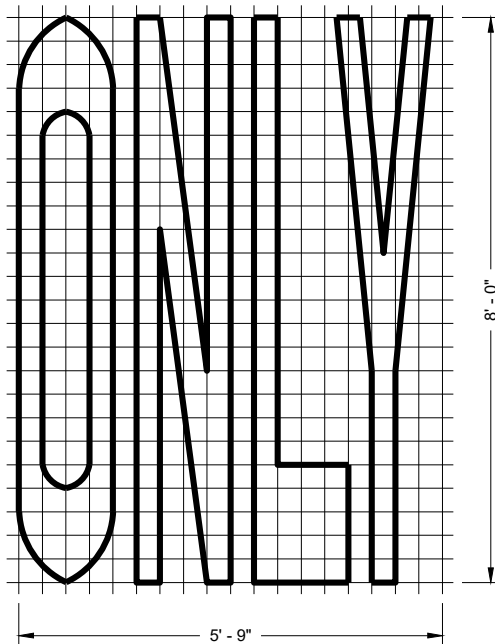
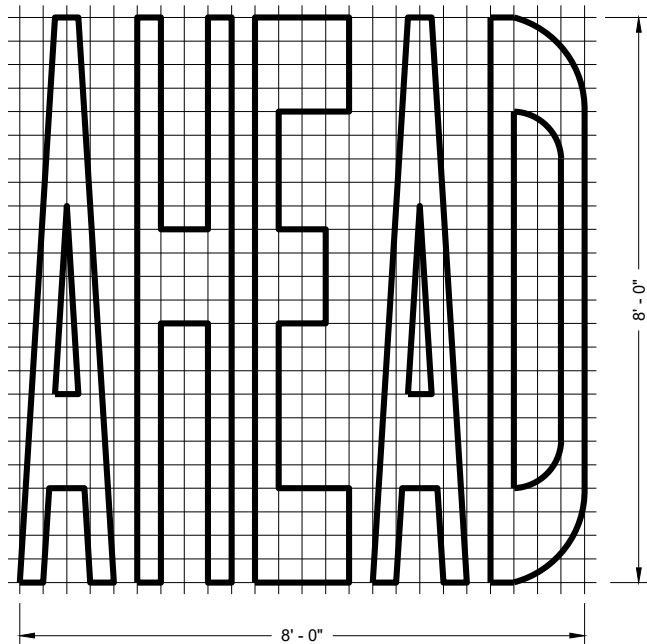
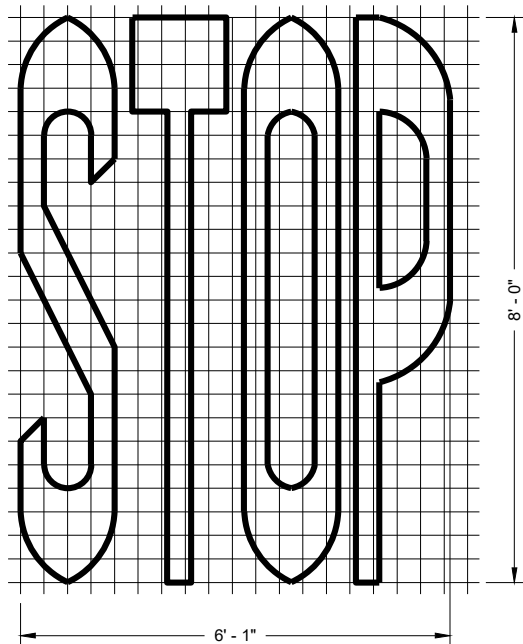
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

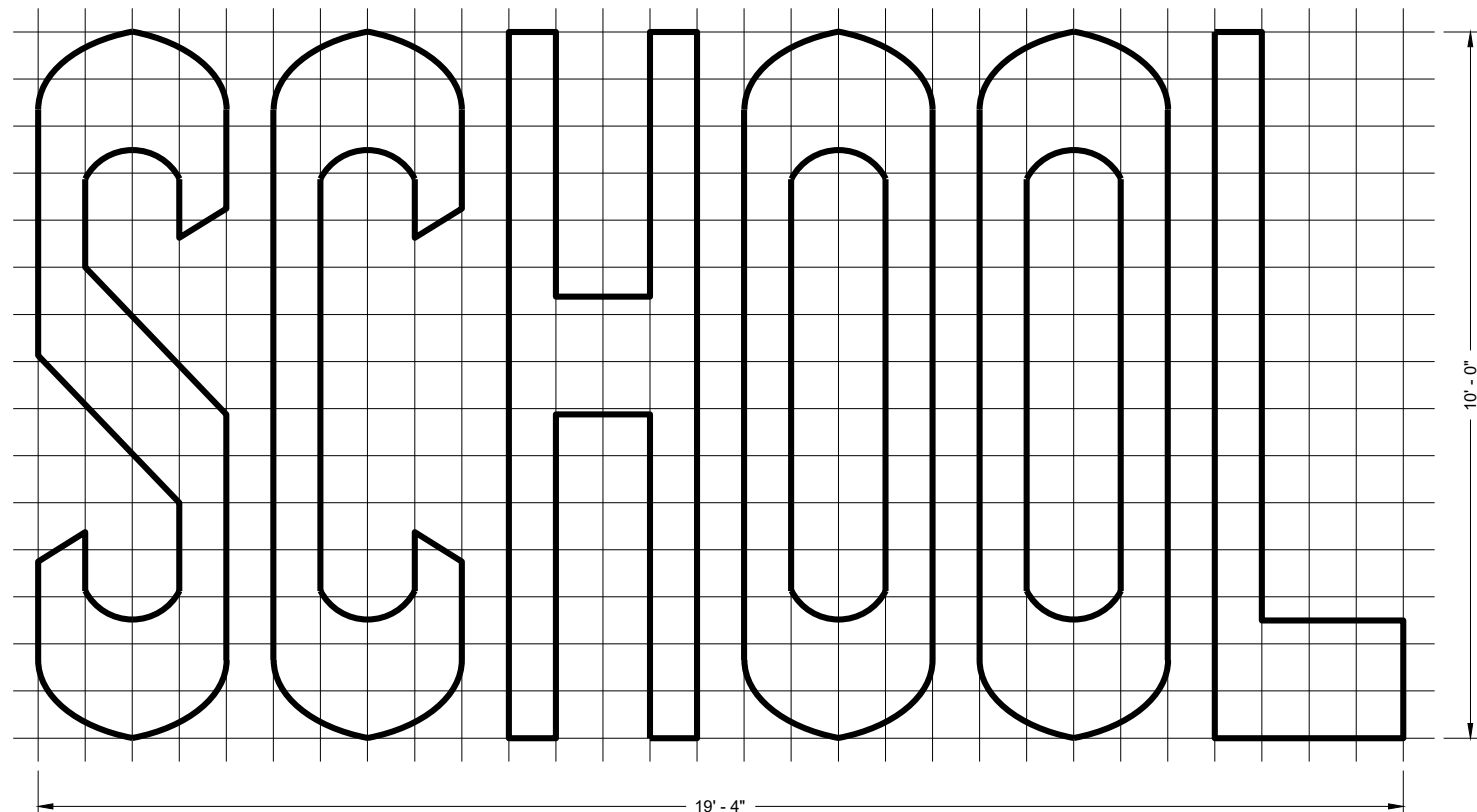
June 2017
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER

FHWA



SINGLE LANE



TWO - LANE

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

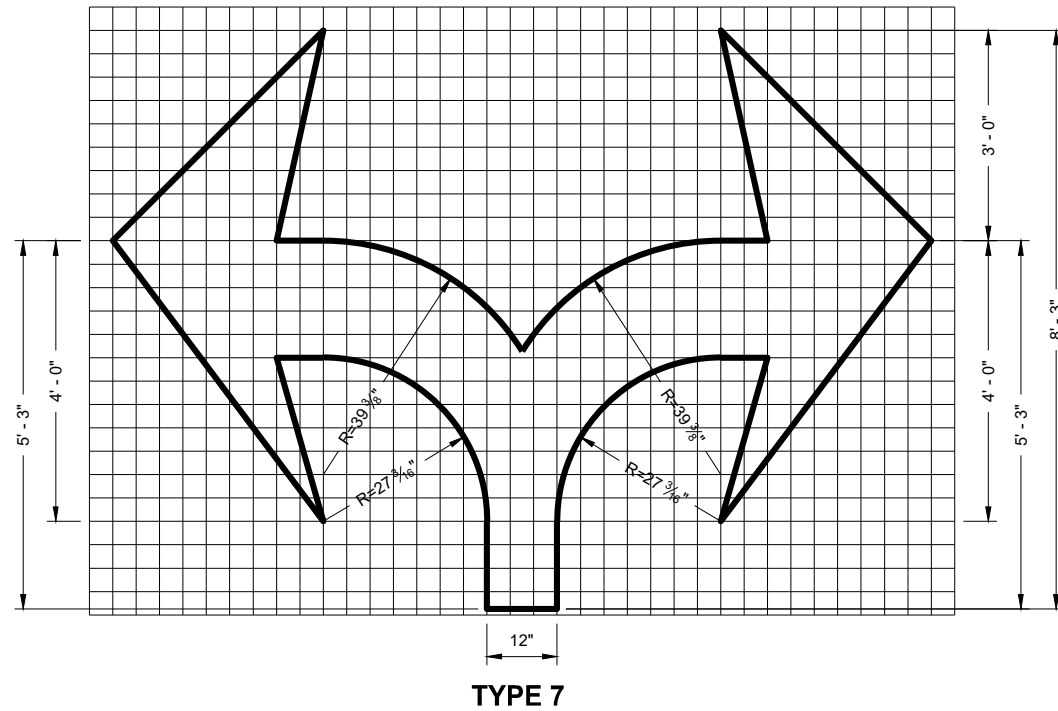
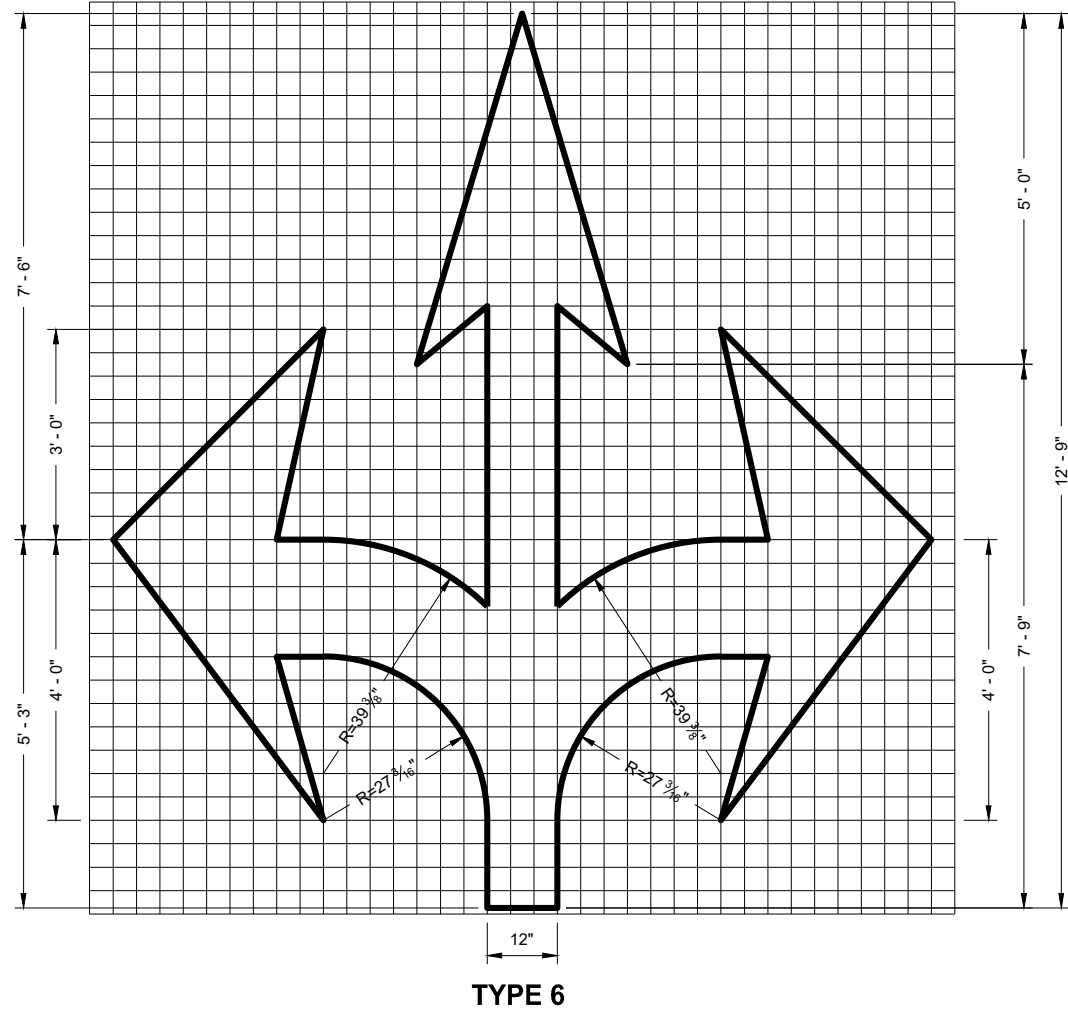
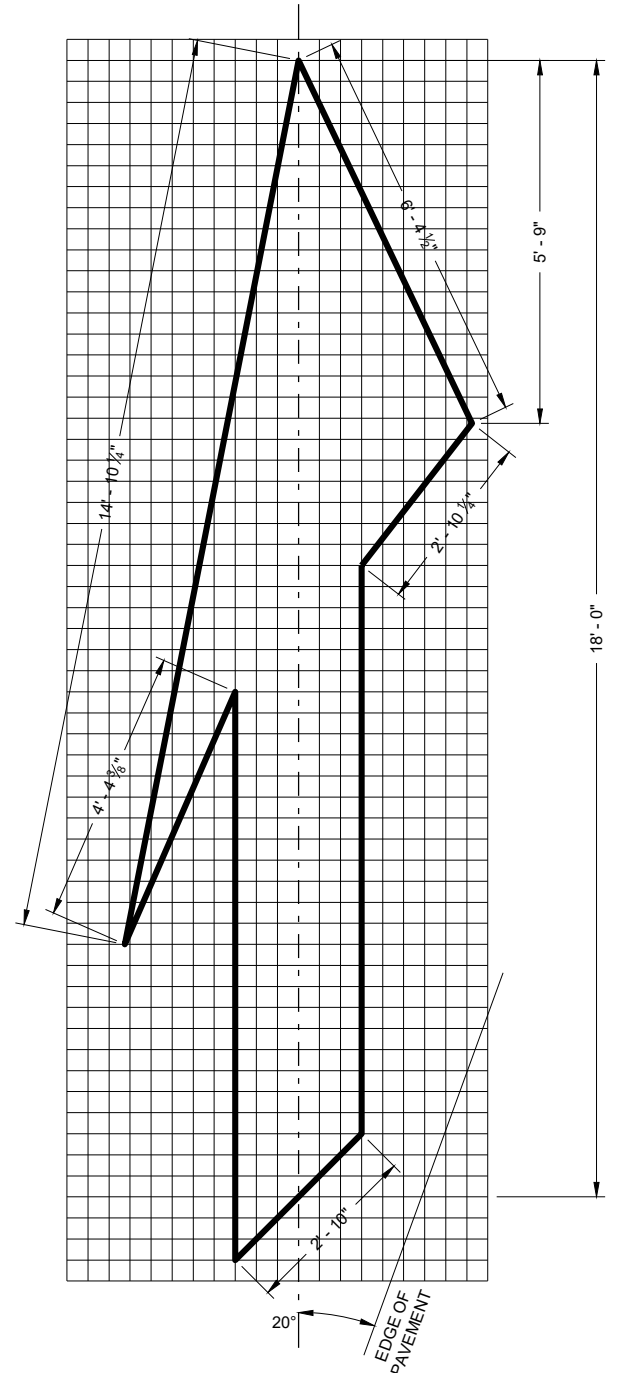
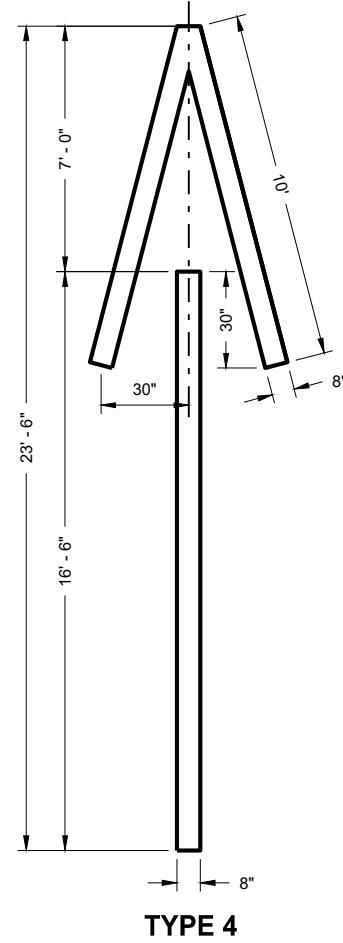
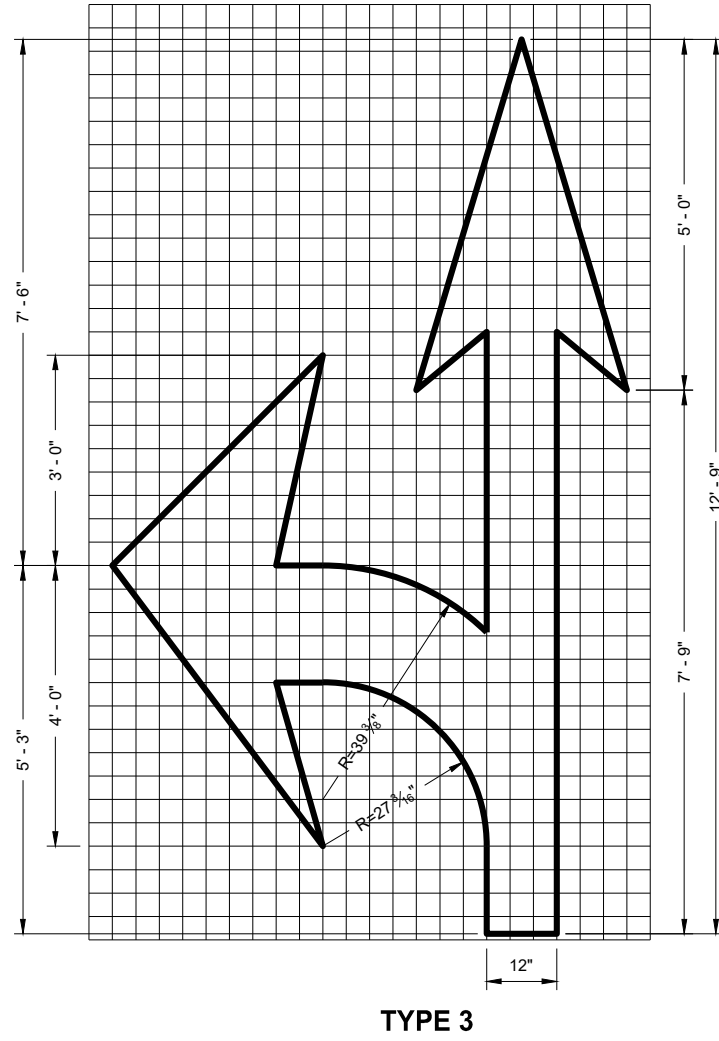
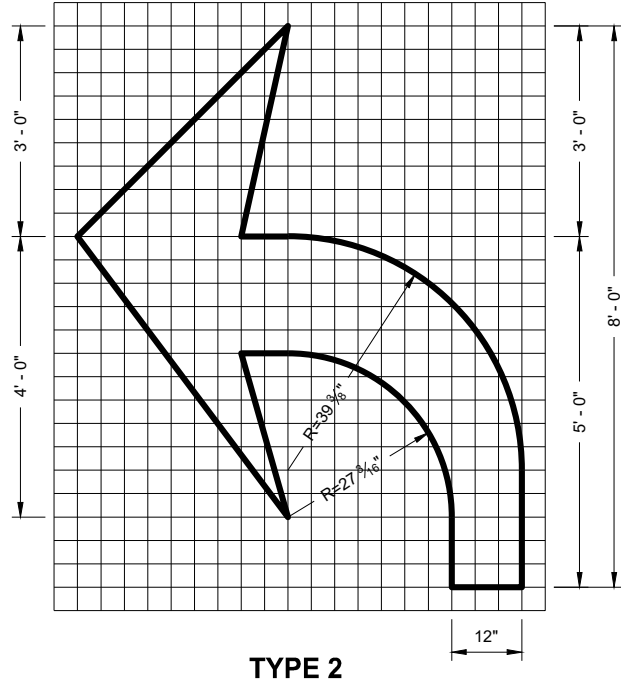
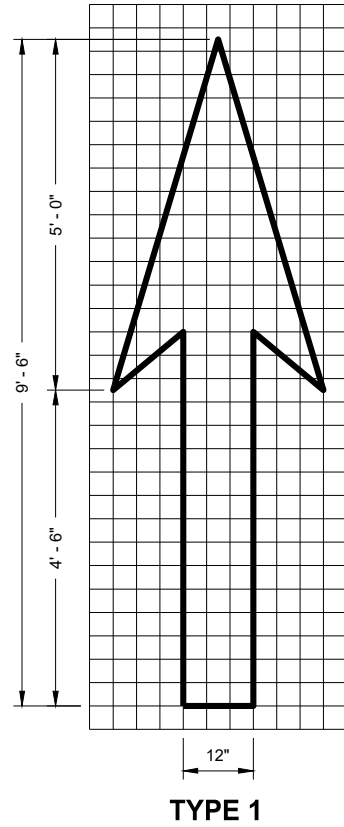
PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

FHWA



GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE
/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER
FHWA

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

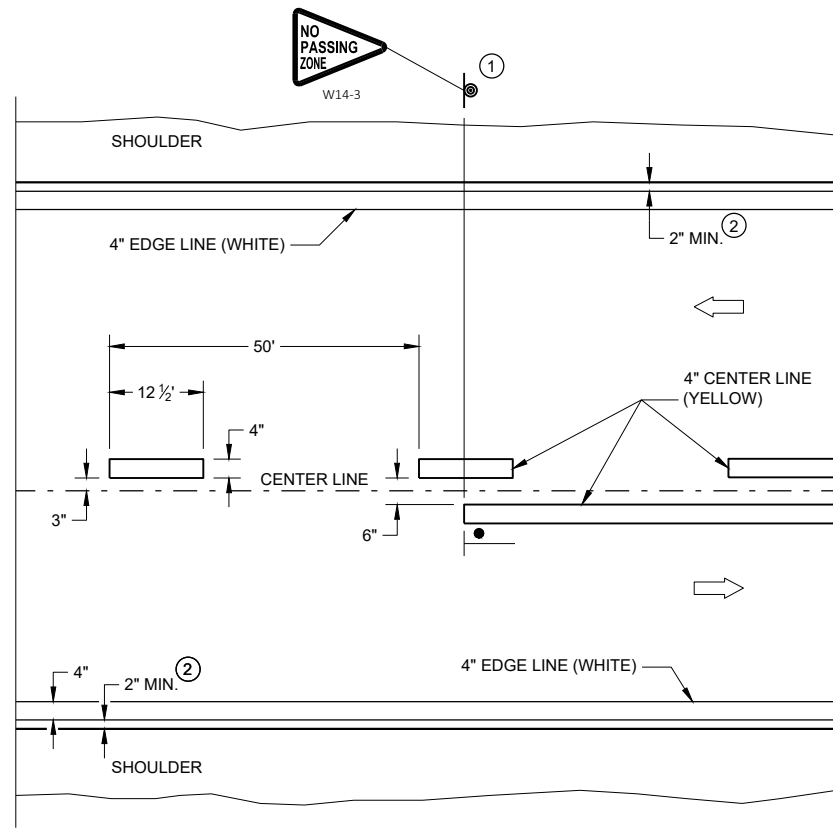


ROUNDAABOUT MARKING ARROWS

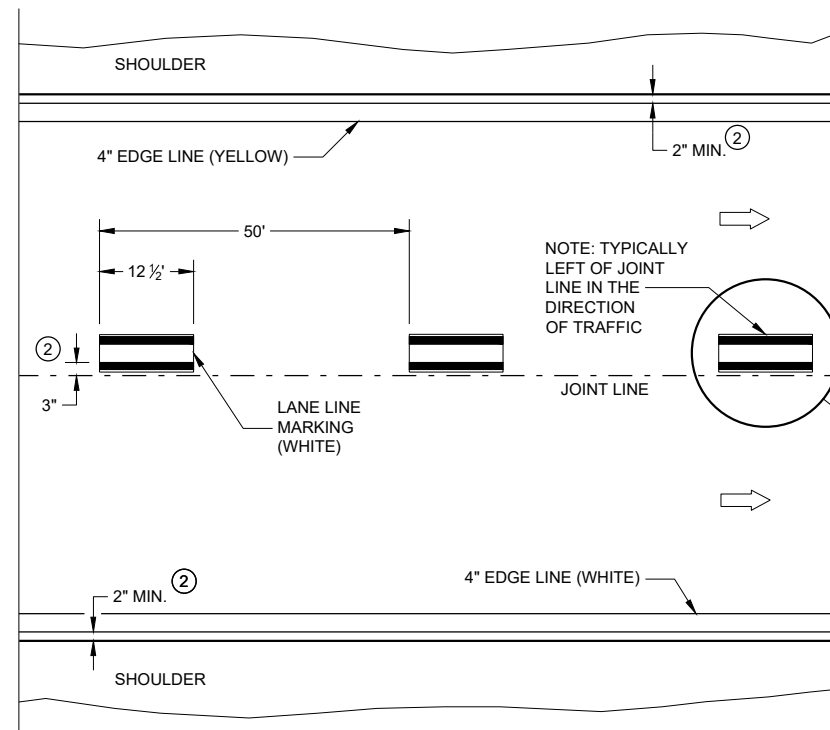
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

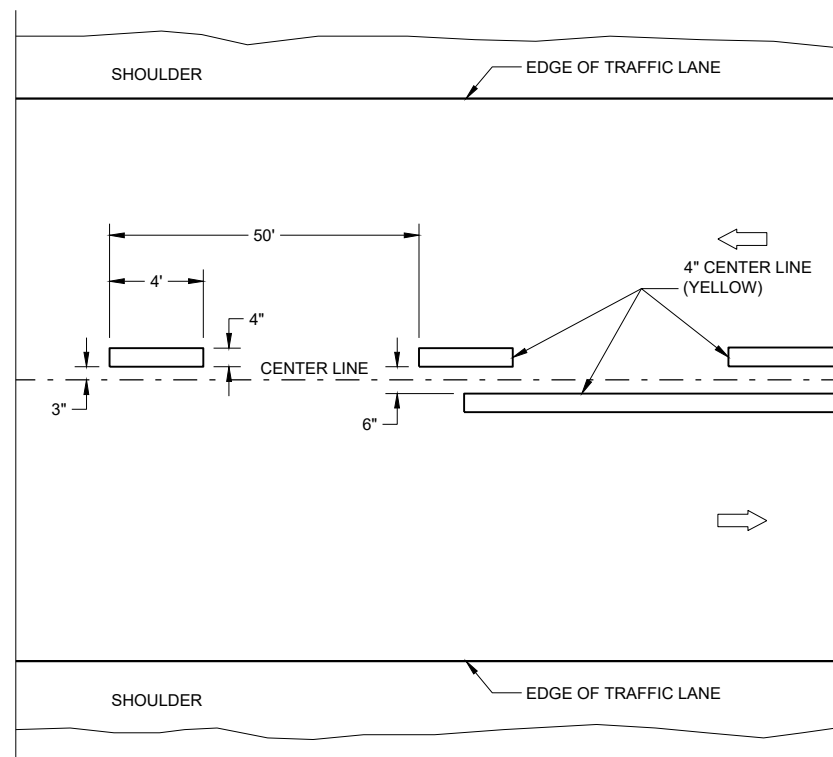


TWO WAY TRAFFIC

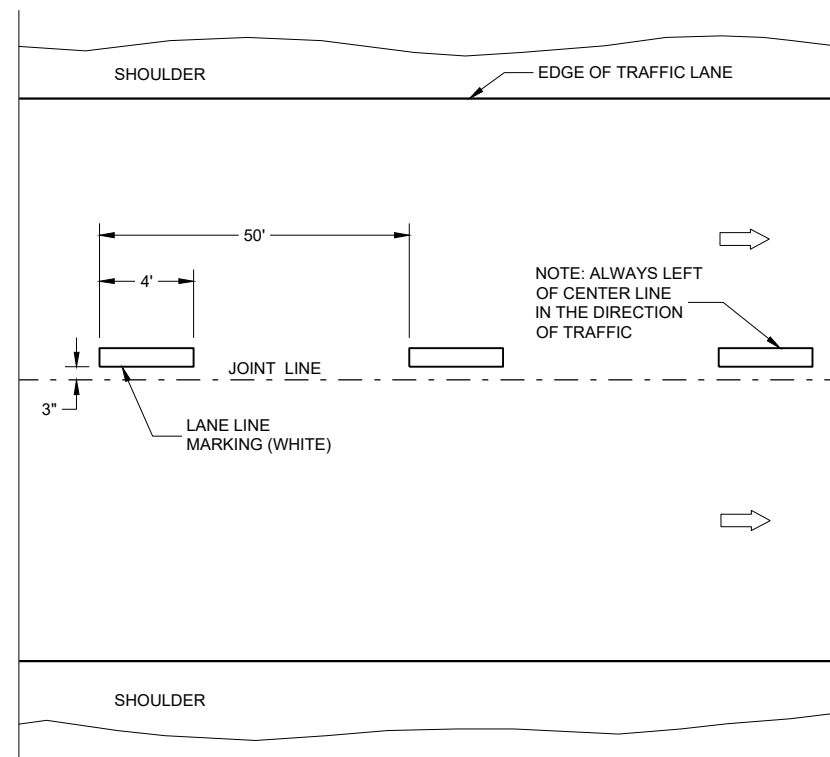


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC




TEMPORARY PAVEMENT MARKING

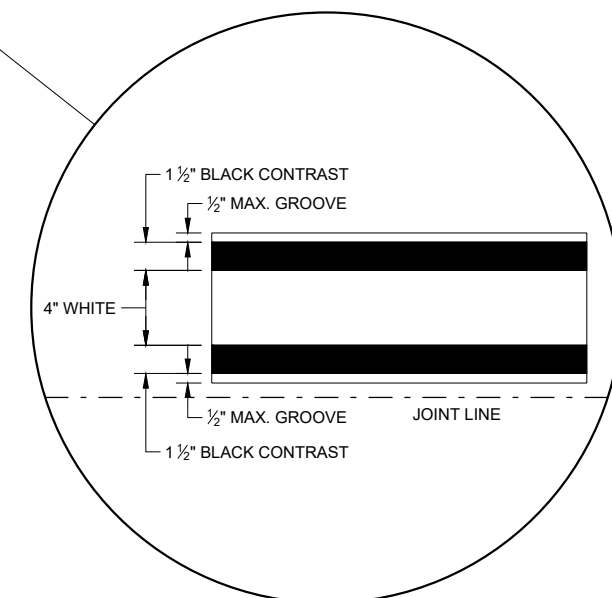
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITH 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
 SIGN ON PERMANENT SUPPORT
 DIRECTION OF TRAFFIC

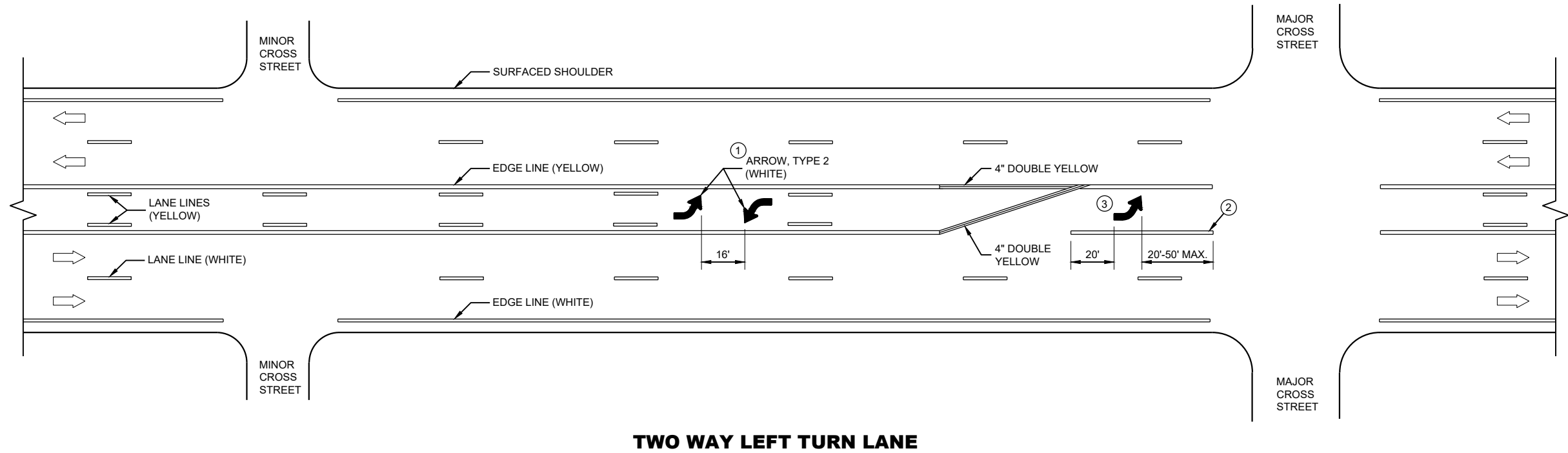


LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

/S/ Matthew Rauch
STATEWIDE SIGNING AND MARKING
ENGINEER



GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 8" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

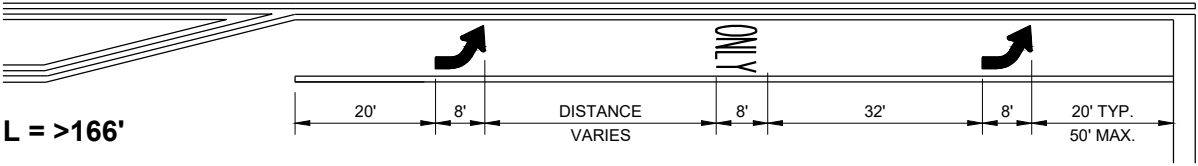
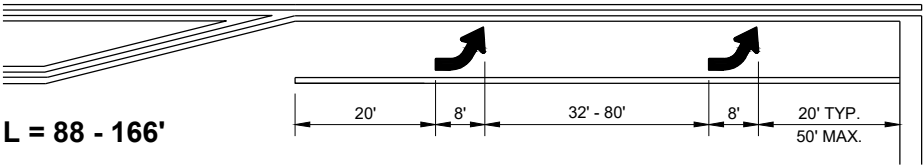
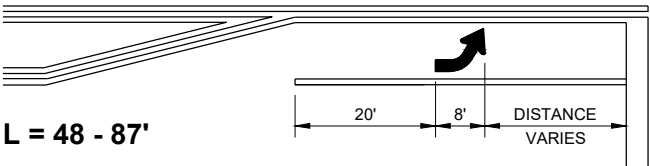
➡ DIRECTION OF TRAFFIC

**PAVEMENT MARKING
(TURN LANES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

TURN LANE OPTIONS

LENGTH OF TURN BAY (L) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE






* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED <u>June 2017</u> DATE	<u>/S/ Andrew Heidtke</u> WORK ZONE ENGINEER

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

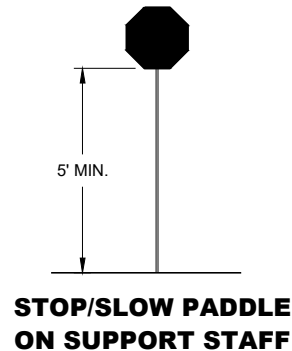
THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
- FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
 - SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.
- TEMPORARY PORTABLE RUMBLE STRIPS**
- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

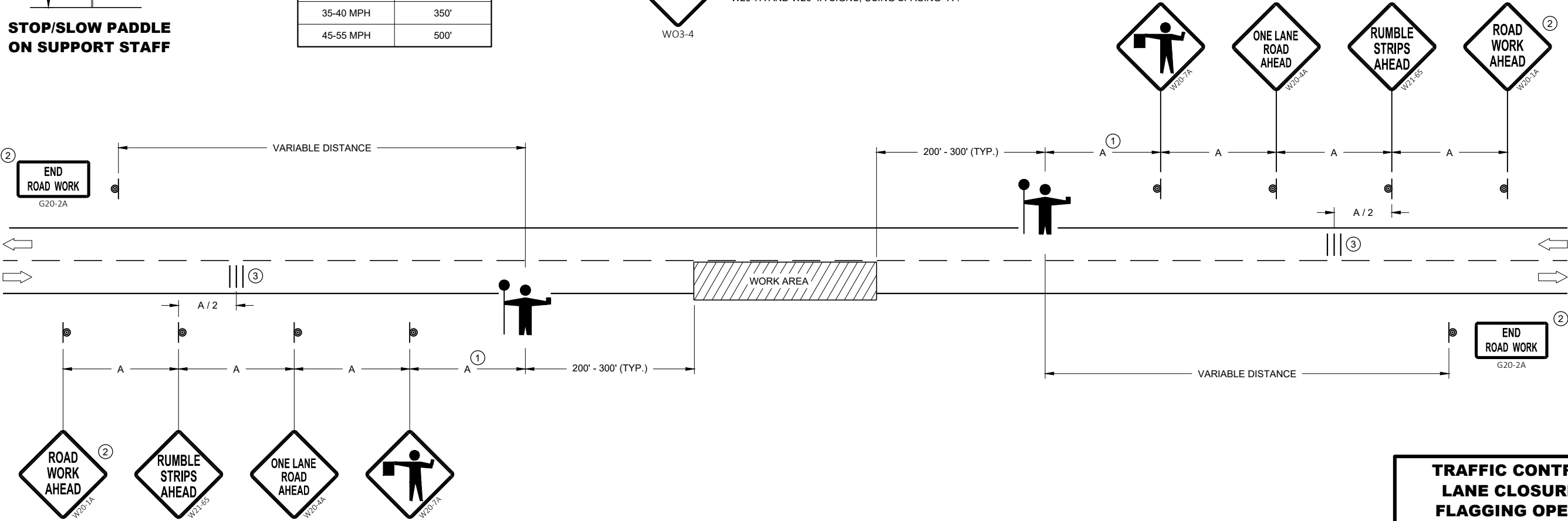


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

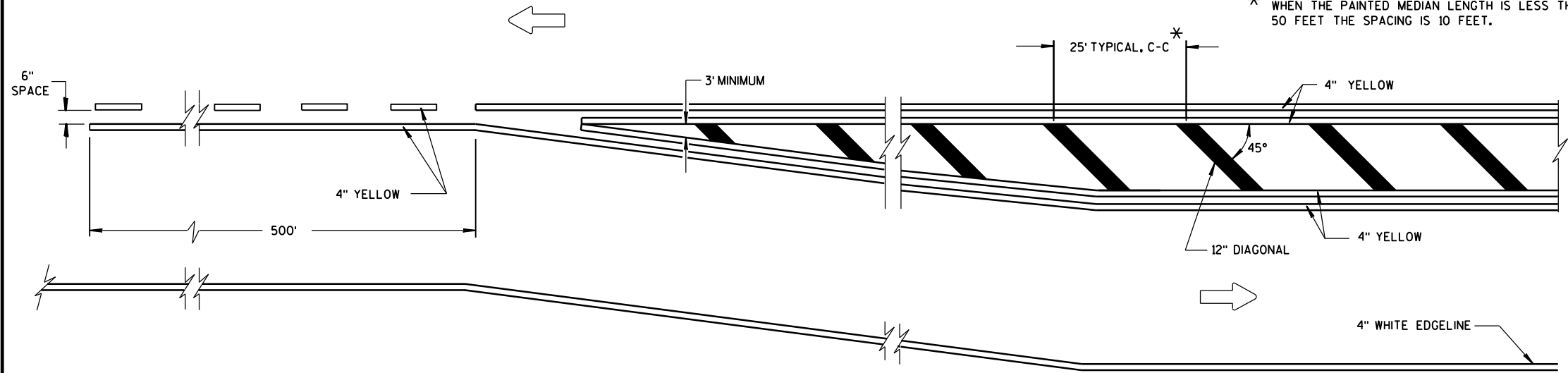


USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

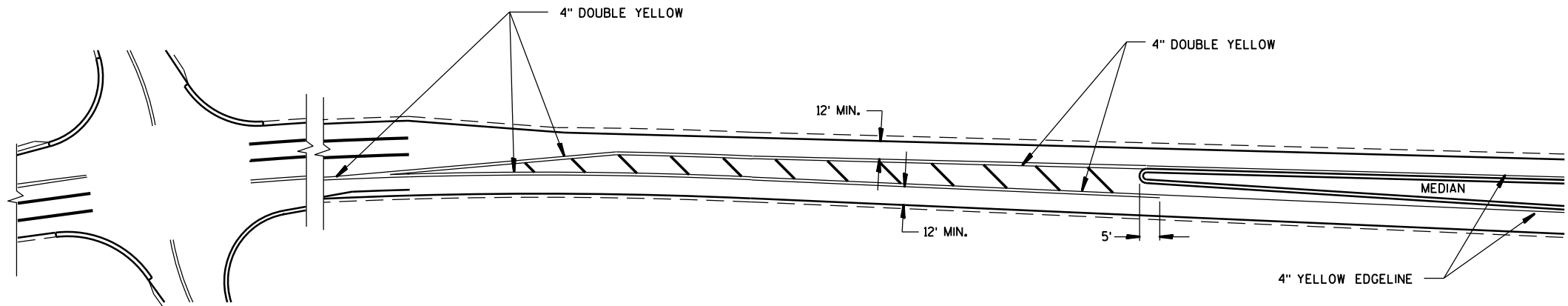


MEDIAN ISLAND DETAIL

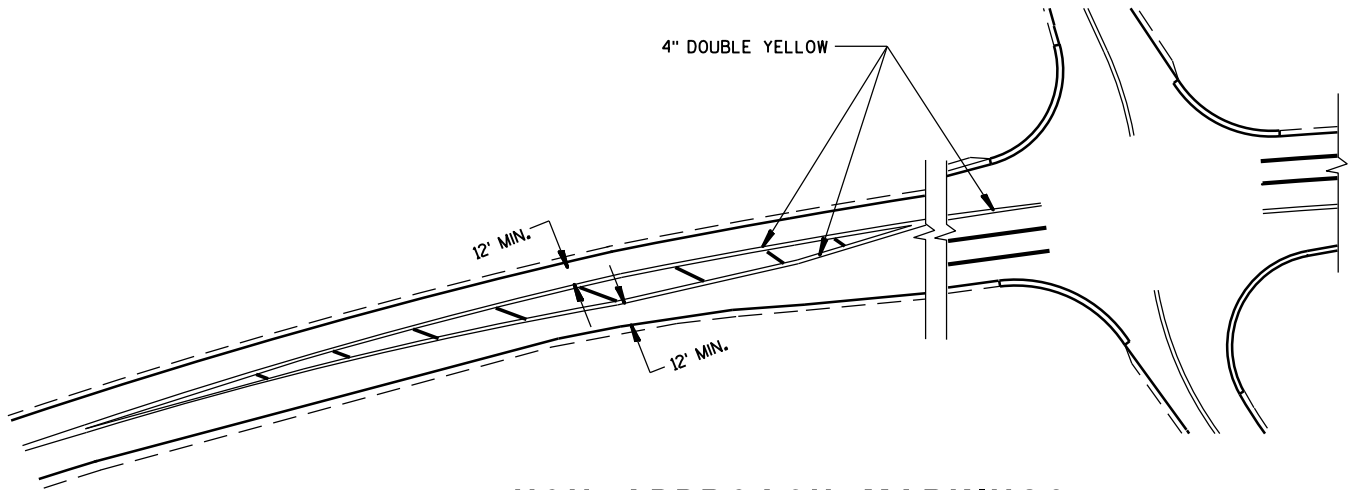
GENERAL NOTE

DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT WIDEST POINT.

➡ DIRECTION OF TRAVEL







APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON APPROACH MARKINGS

MEDIAN ISLAND MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER FHWA

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

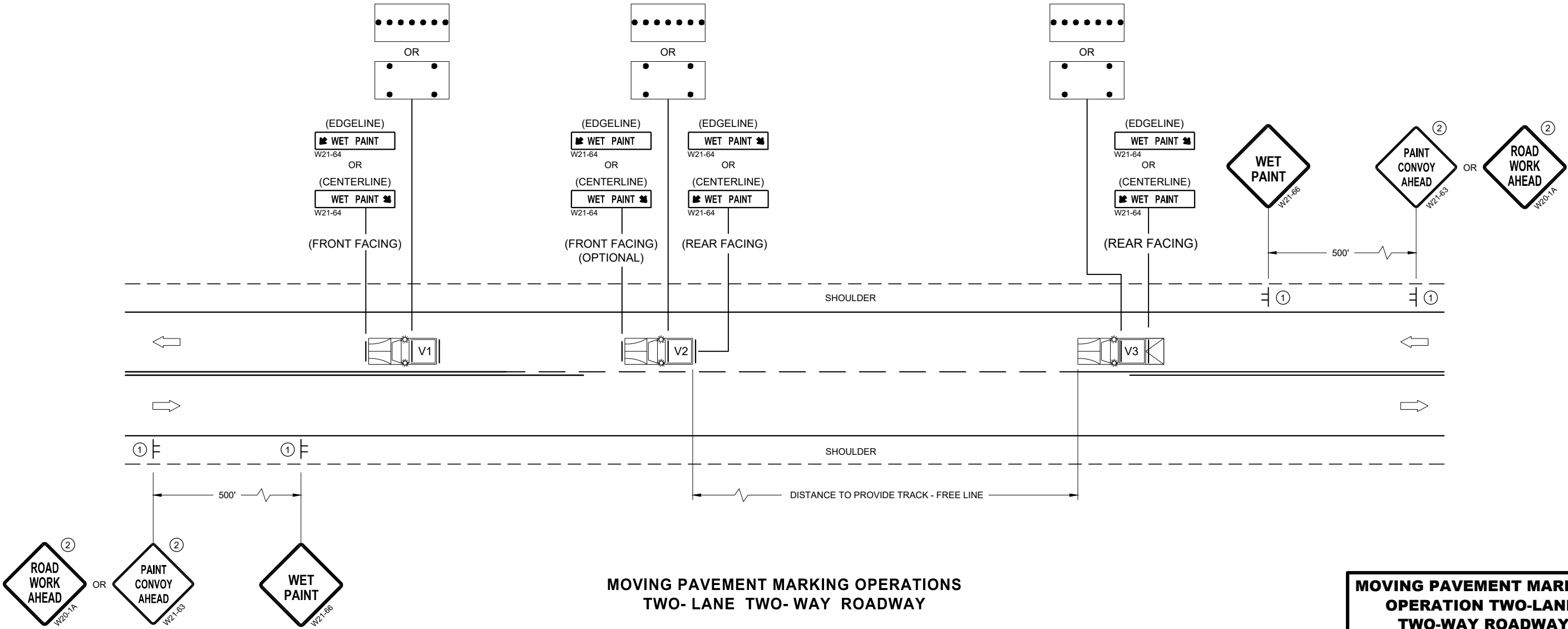
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 18" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.



MOVING PAVEMENT MARKING
OPERATION TWO-LANE
TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

V1	LEAD VEHICLE
V2	MARKING VEHICLE
V3	SHADOW VEHICLE



TRUCK MOUNTED ATTENUATOR (TMA)



SIGN ON TEMPORARY SUPPORT



DIRECTION OF TRAFFIC



FLASHING ARROW PANEL (MERGE)



FLASHING ARROW PANEL (CAUTION)

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

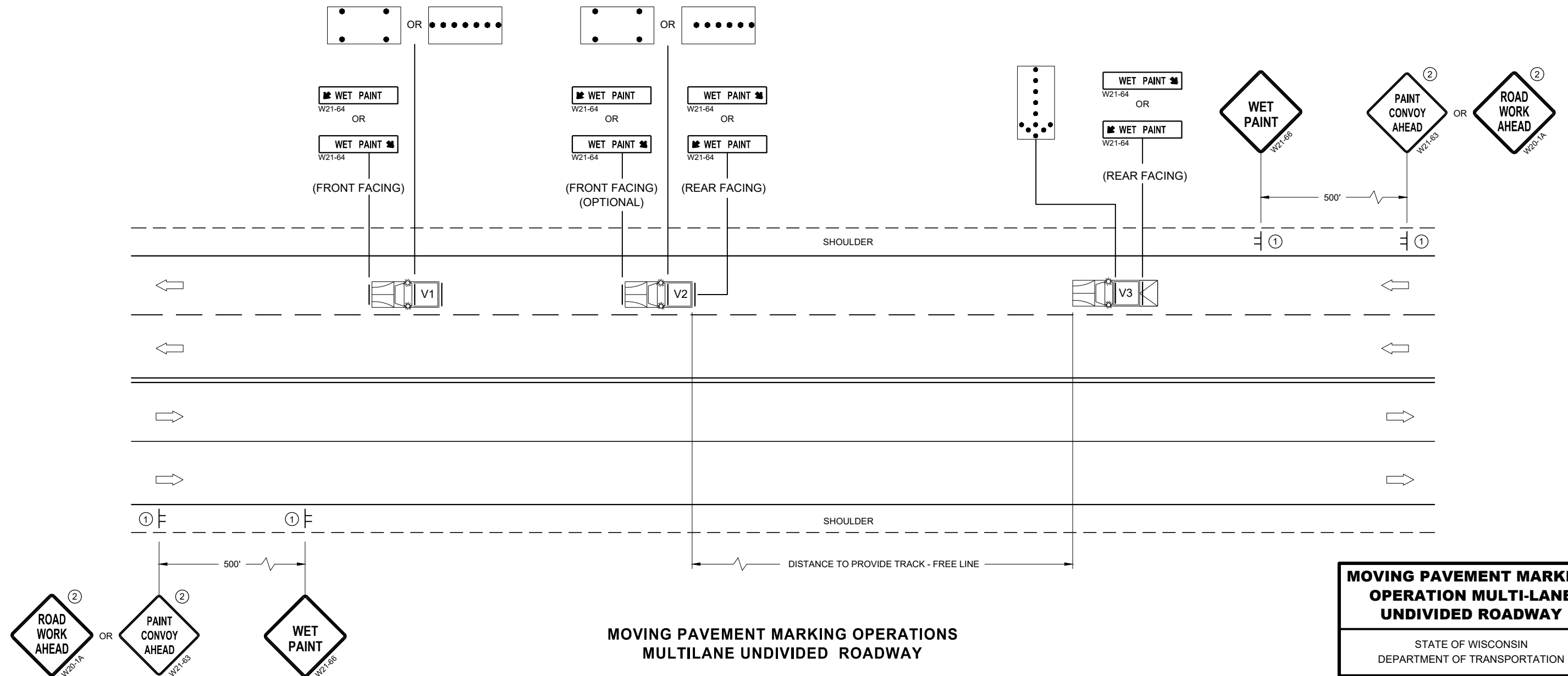
WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLES AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL HAVE A MINIMUM HEIGHT OF 18" FOR WET PAVEMENT MARKINGS.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.



MOVING PAVEMENT MARKING OPERATIONS MULTILANE UNDIVIDED ROADWAY

MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

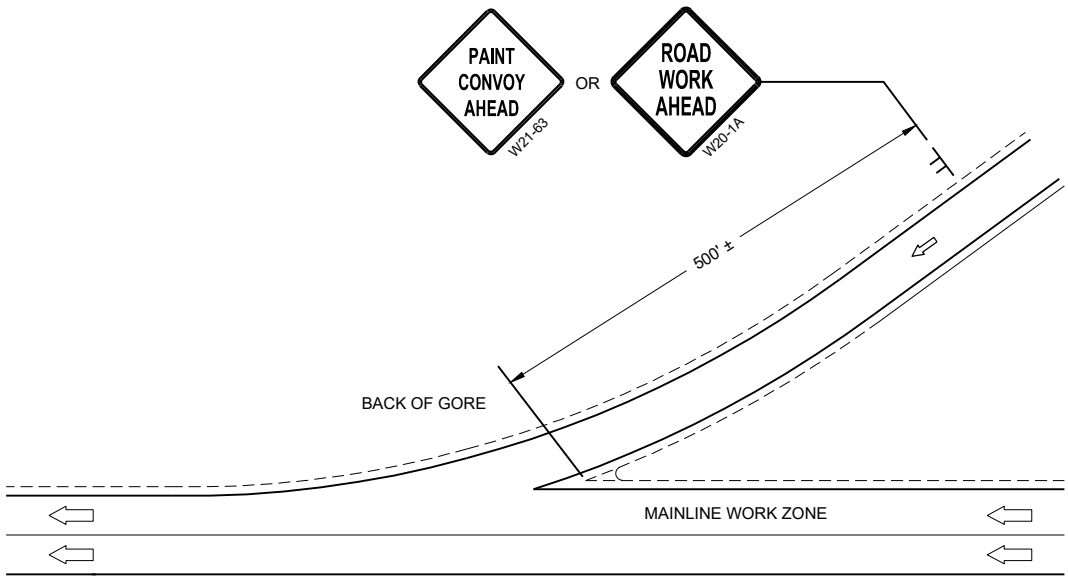
APPROVED
November 2019
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

LEGEND

- V1 MARKING VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE
- TRUCK MOUNTED ATTENUATOR (TMA)
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- FLASHING ARROW PANEL (MERGE)
- FLASHING ARROW PANEL (CAUTION)



GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

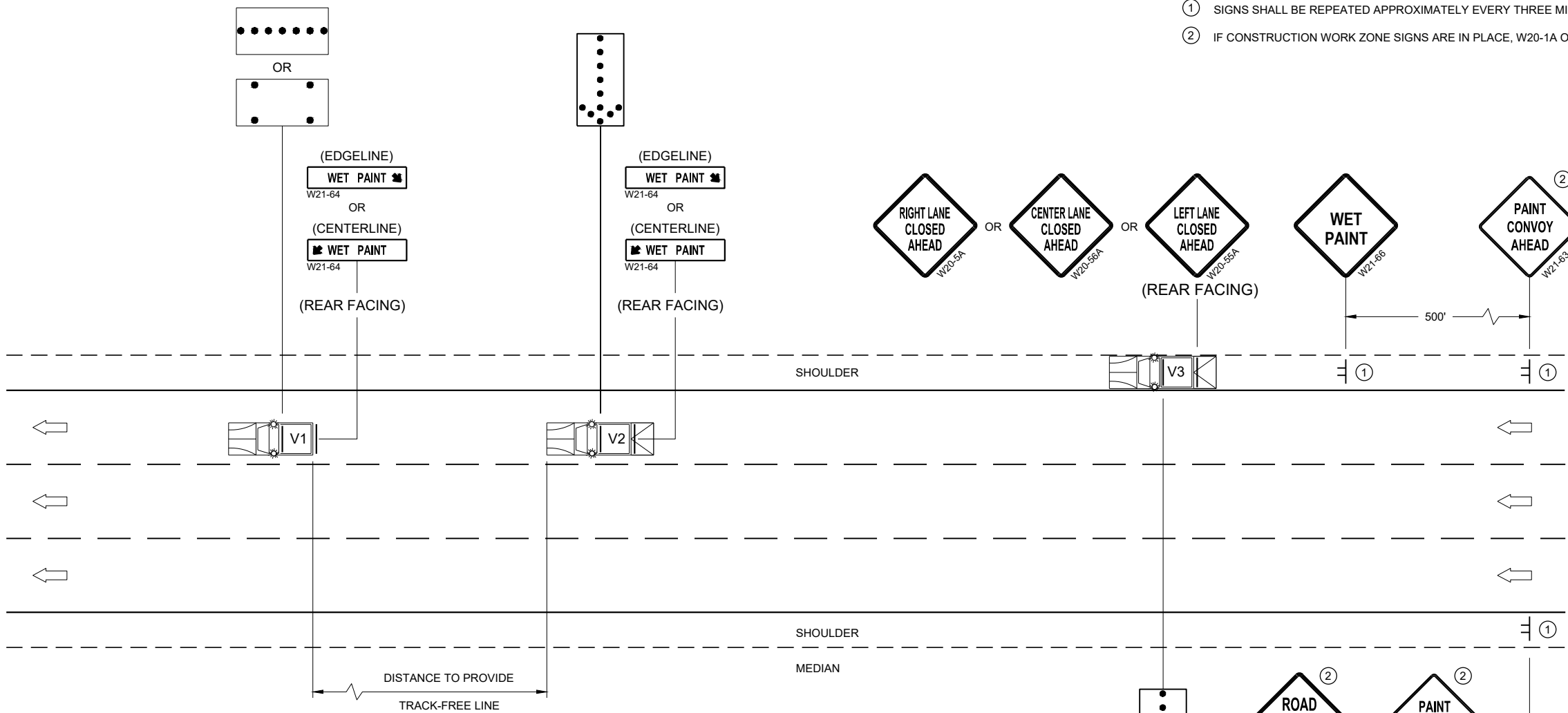
IF THE SHOULDER IS TOO NARROW TO ACCOMMODATE THE LAST TRAILING VEHICLE, THE VEHICLE SHOULD STRADDLE THE EDGE LINE.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM HEIGHT OF 18" FOR WET PAVEMENT MARKINGS

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY ON RAMP.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.



MOVING PAVEMENT MARKING OPERATIONS
MULTILANE DIVIDED ROADWAY

MOVING PAVEMENT MARKING
OPERATION MULTI-LANE
DIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

SIGNING AND MARKING IS SHOWN AS TYPICAL PLACEMENT. FIELD CONDITIONS MAY DICTATE CHANGES IN SIGNING AND MARKING PLACEMENT.

① USED ONLY WHEN APPROVED BY REGION TRAFFIC ENGINEER.

* SIGNS MAY BE OMITTED IF SPACE DOES NOT PERMIT PLACEMENT.

** IF POSTED SPEED IS 45 MPH OR GREATER, PLACE W5-54 SIGN UNDER R4-7 SIGN. MOUNT W5-54 SIGN AT 4' MOUNTING HEIGHT (TOP OF ROADWAY TO BOTTOM OF SIGN).

LEGEND

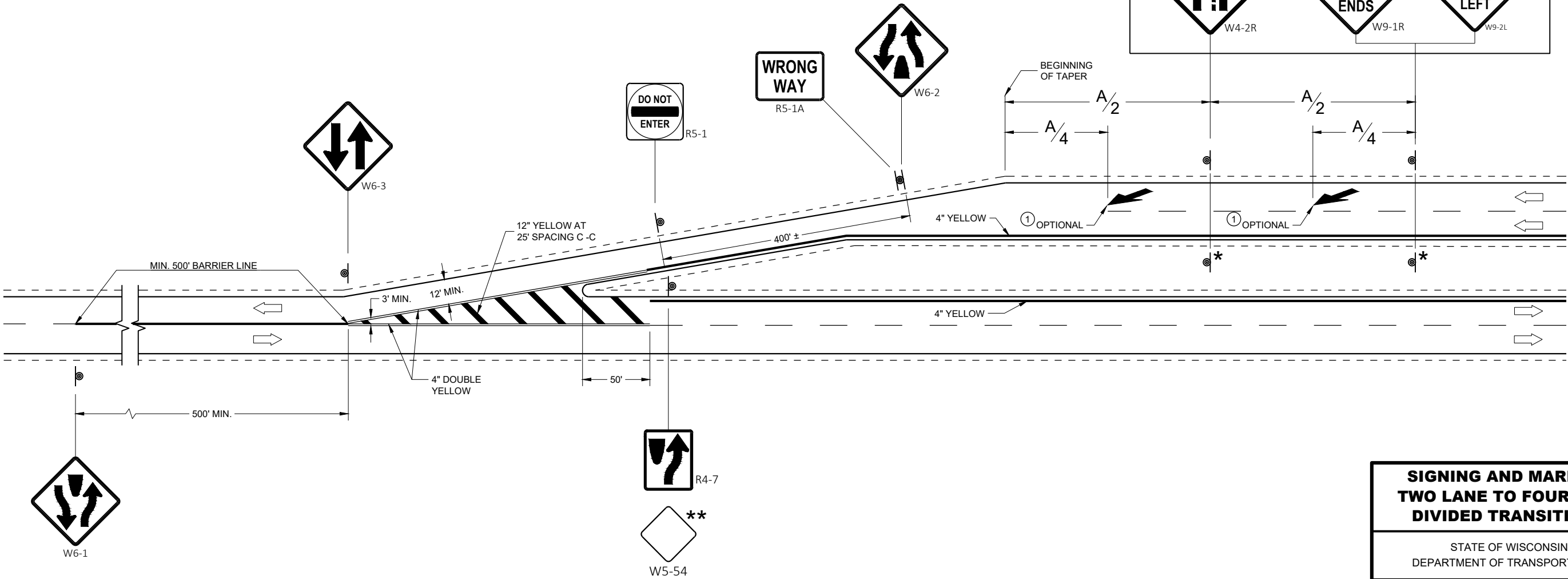
A DISTANCE DEPENDENT ON SPEED (SEE TABLE)

⦿ SIGN MOUNTED ON PERMANENT SUPPORT

➡ DIRECTION OF TRAFFIC

DISTANCE TABLE

POSTED OR 85TH PERCENTILE SPEED	DISTANCE "A"
25	325'
30	460'
35	565'
40	670'
45	775'
50	885'
55	990'
65	1200'
70	1250'



SIGNING AND MARKING
TWO LANE TO FOUR LANE
DIVIDED TRANSITIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

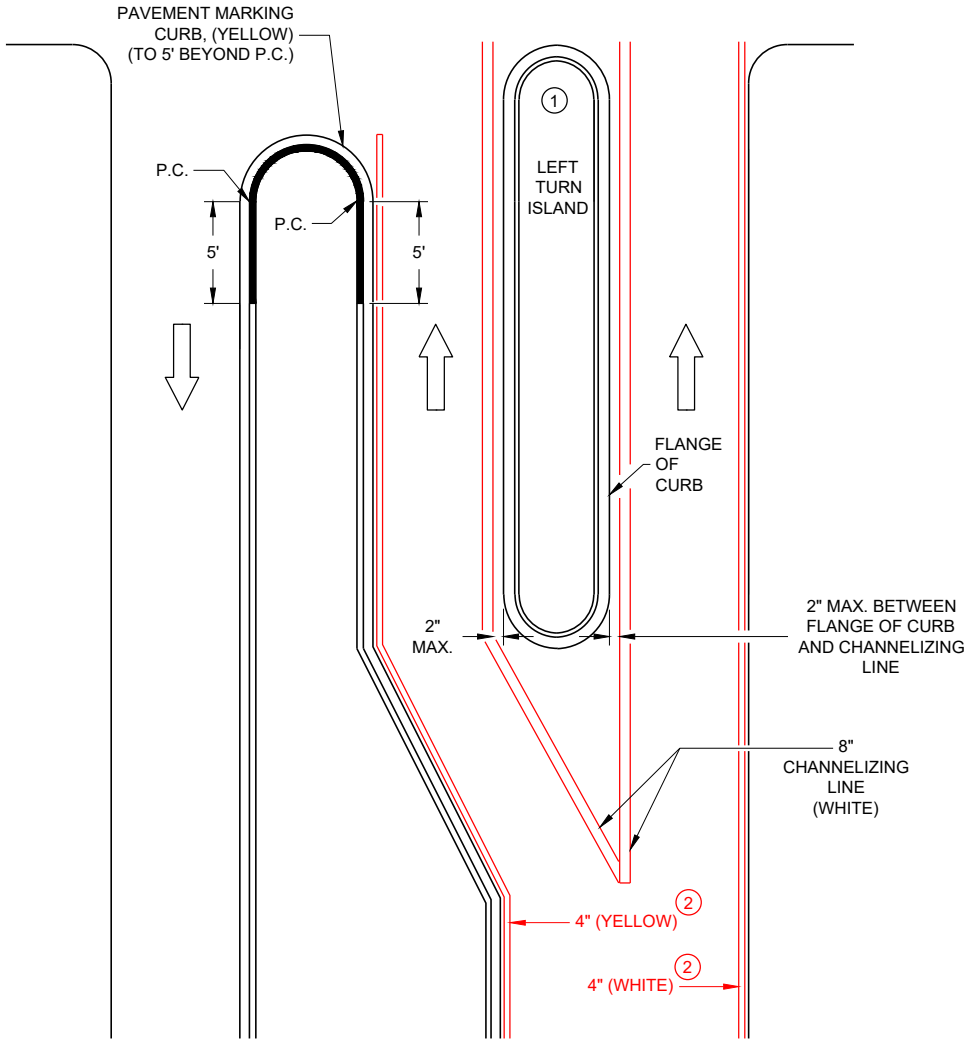
May 2020

DATE

FHWA

/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

REQUIREMENTS FOR EDGE LINES		
POSTED SPEED	IS THERE CONTINUOUS LIGHTING?	
	YES	NO
≤ 30 MPH	NO	OPTIONAL
35 OR 40 MPH	OPTIONAL	RECOMMENDED
≥ 45 MPH	RECOMMENDED	REQUIRED



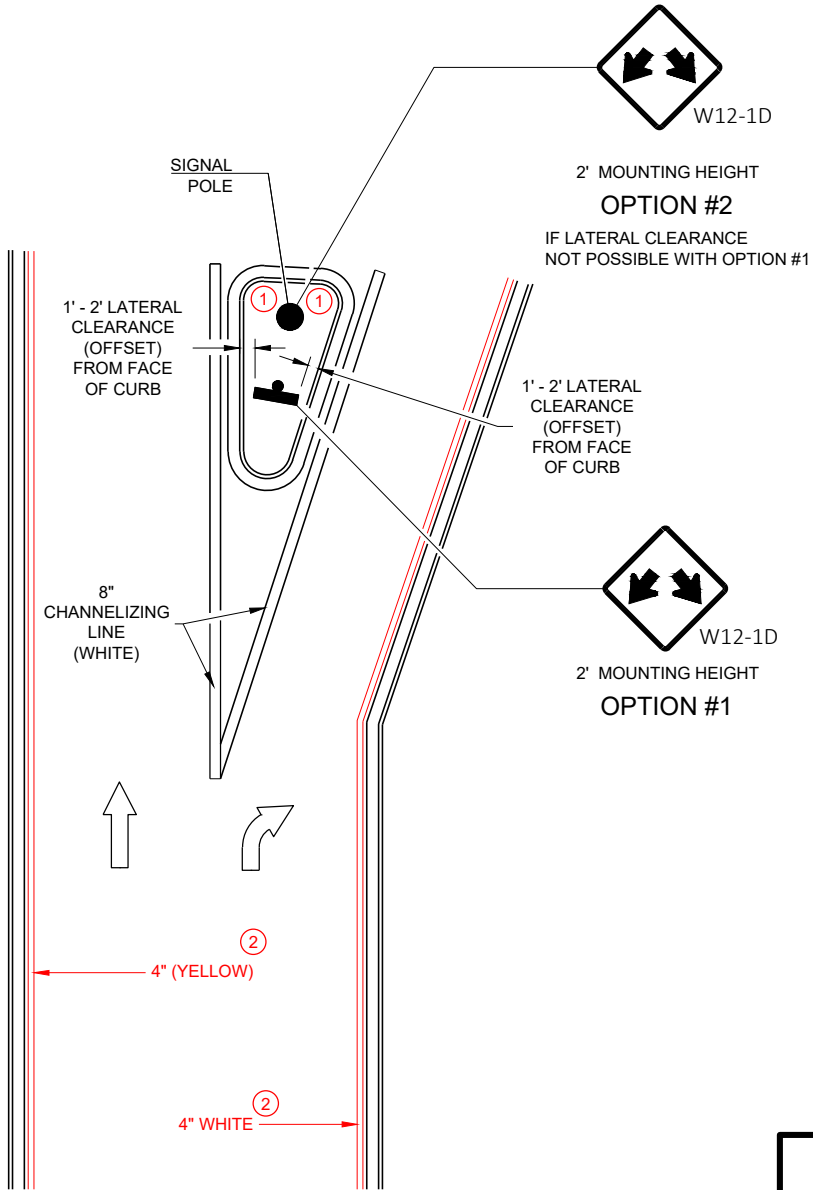
LEFT TURN & MEDIAN ISLAND

GENERAL NOTES

APPLIES TO ISLANDS AT LEFT TURNS AT ONE WAY ROADWAYS AS WELL.
SEE MISCELLANEOUS QUANTITIES FOR SIGN SIZE.

- ① MARK CURB NOSES YELLOW.
- ② MARK ACCORDING TO TABLE.

➡ DIRECTION OF TRAVEL



RIGHT TURN ISLAND

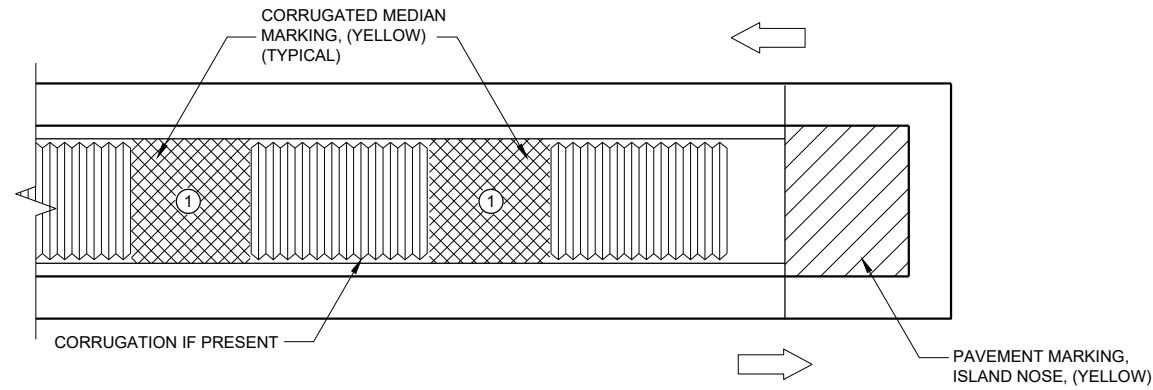
DOUBLE ARROW
WARNING SIGN PLACEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

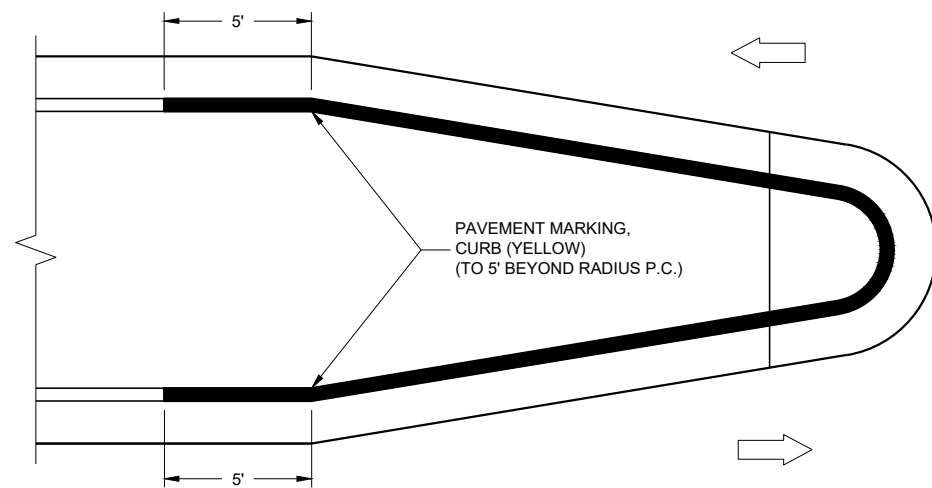
APPROVED
7/2018
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MARKING
ENGINEER

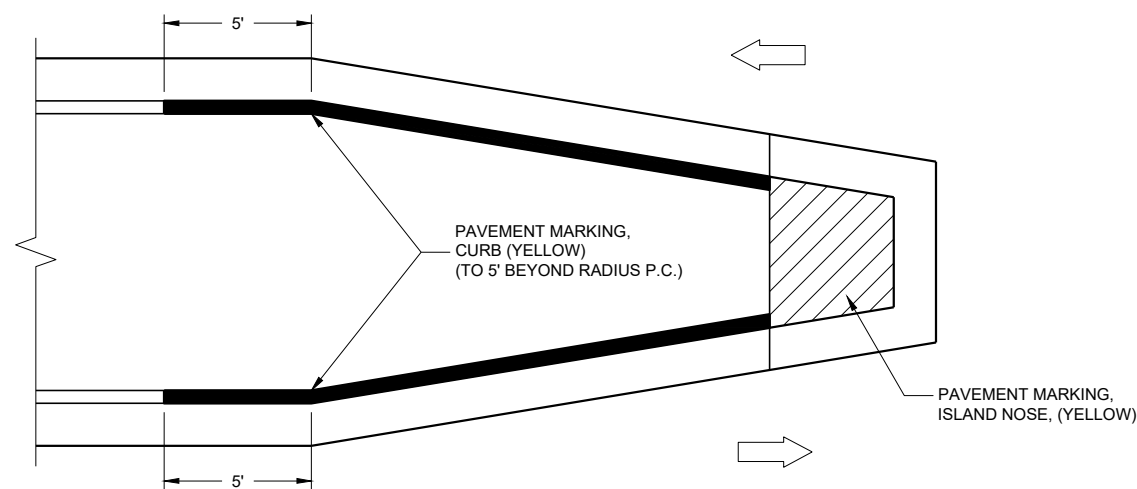
FHWA



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

- ISLAND NOSE MARKING
- CURB MARKING
- CORRUGATED MEDIAN MARKING
- DIRECTION OF TRAVEL

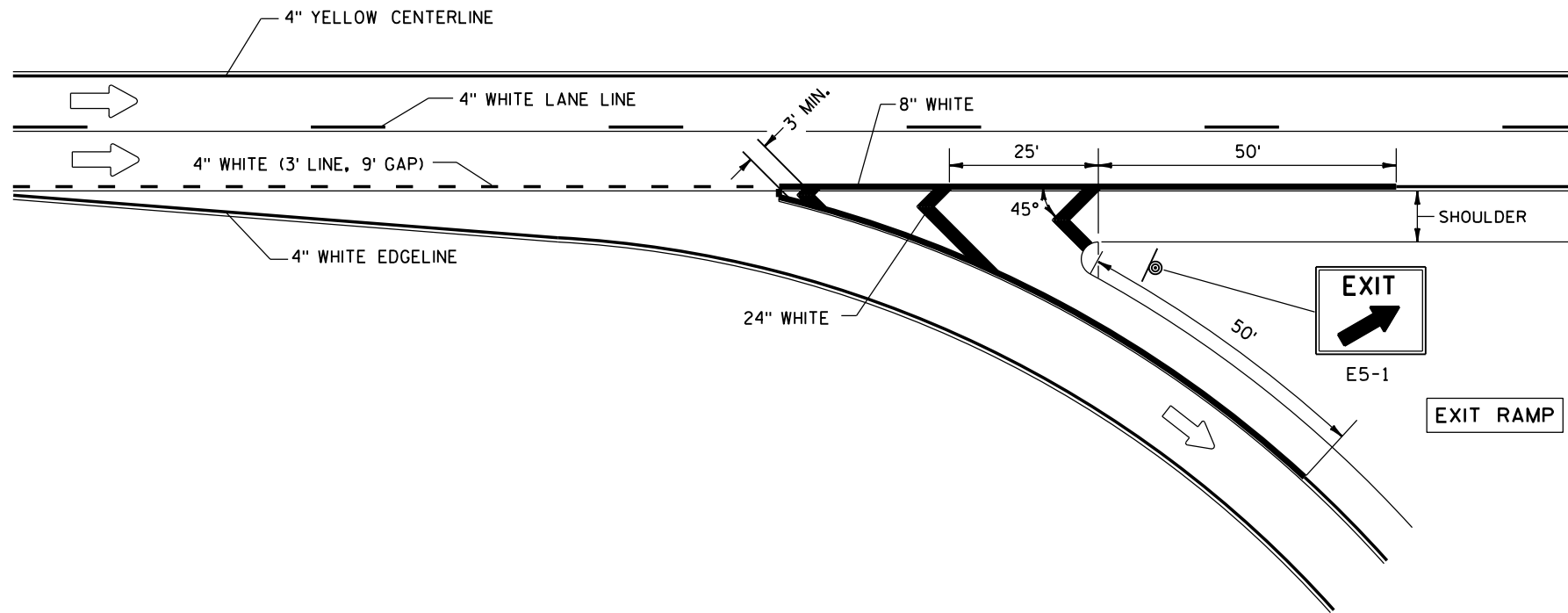
PAVEMENT MARKINGS (ISLANDS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MARKING
ENGINEER

FHWA



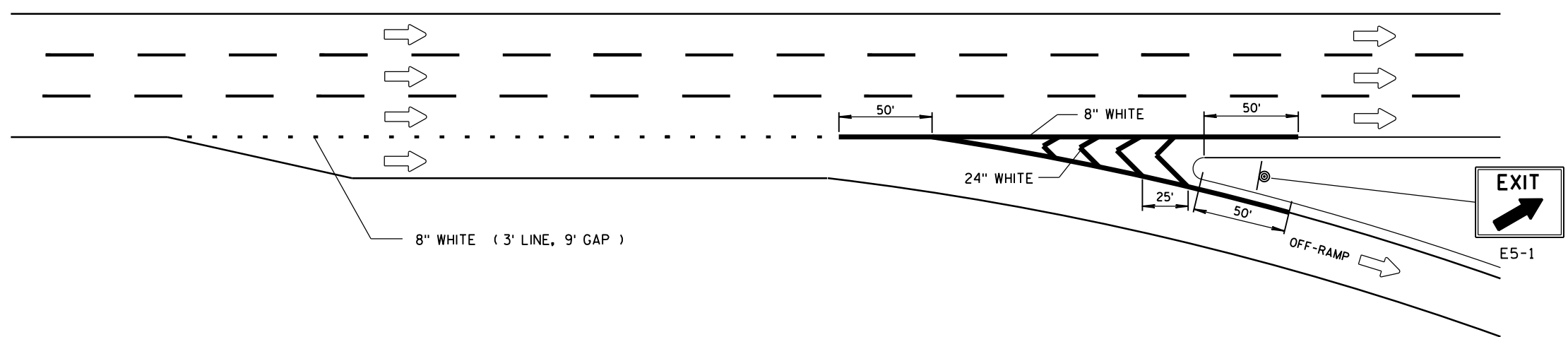
GENERAL NOTES

PLACE GROOVE 3 INCHES LEFT OF JOINT.

LEGEND

- DIRECTION OF TRAVEL
- SIGN ON PERMANENT SUPPORT

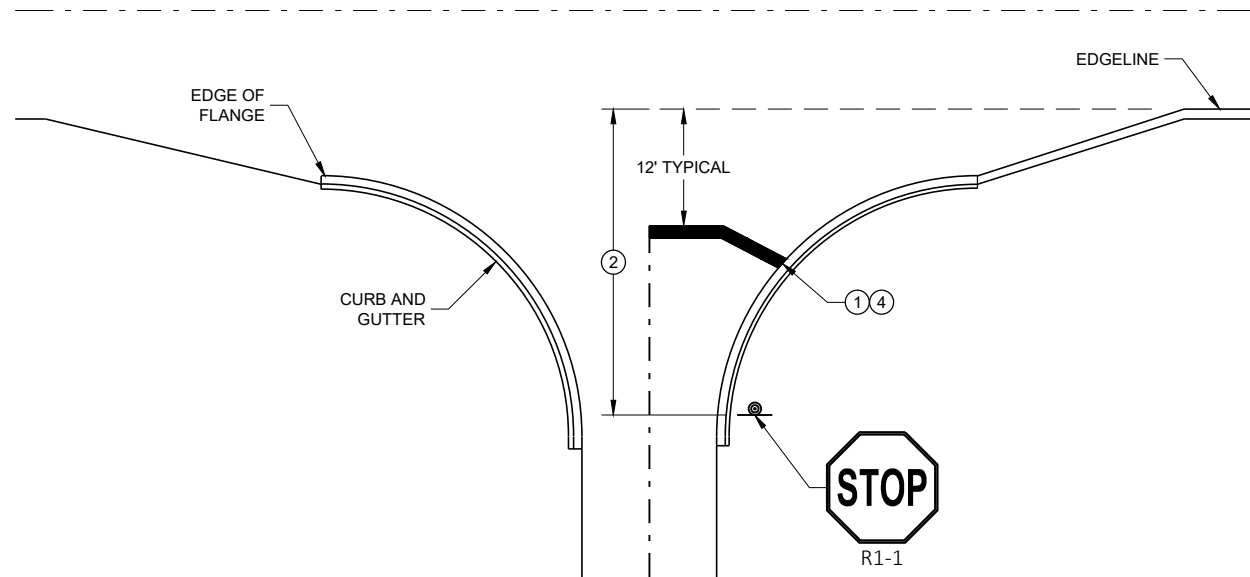
PAVEMENT MARKING FOR EXIT RAMP



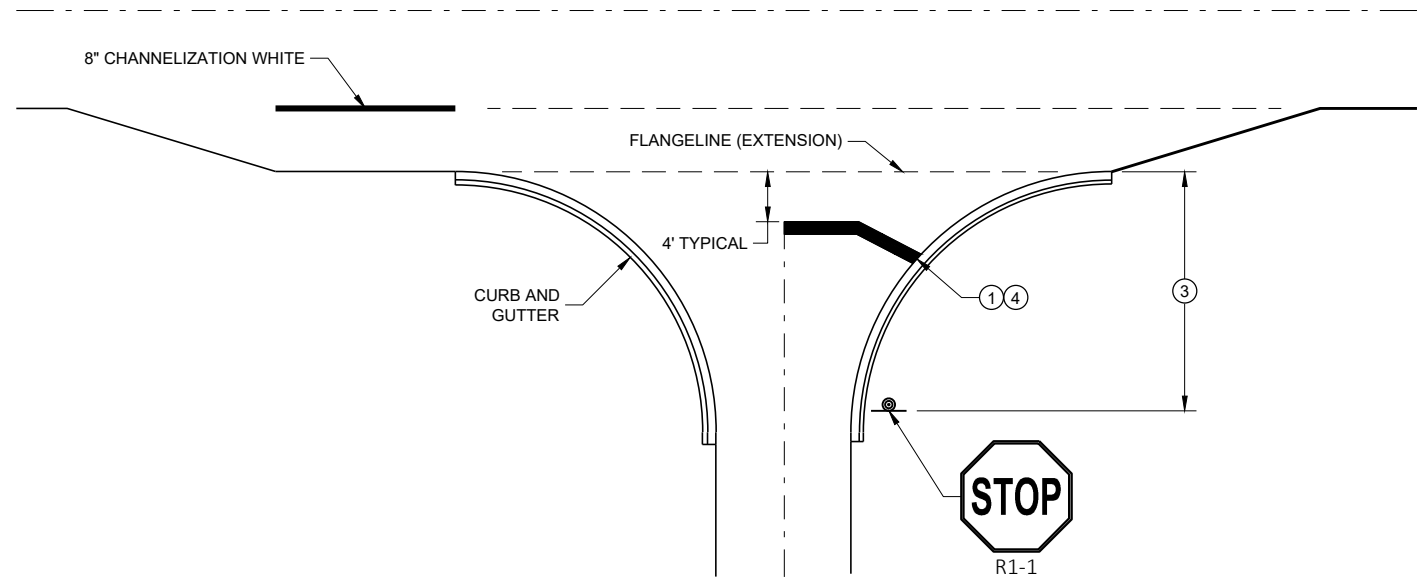
SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL EXIT-RAMP

PAVEMENT MARKING
(RAMPS AND GOES)

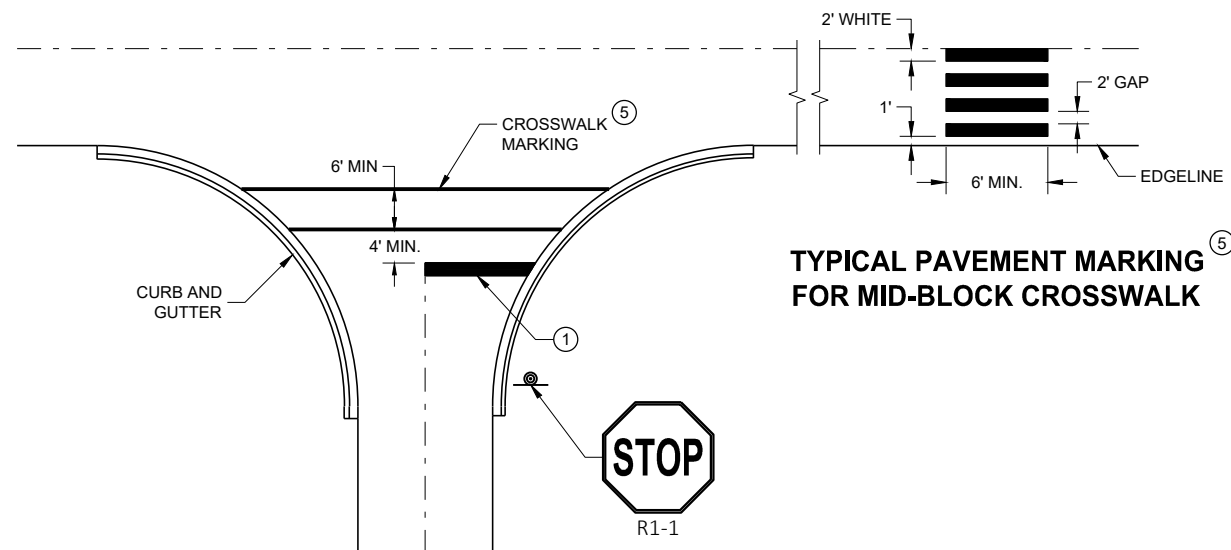
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



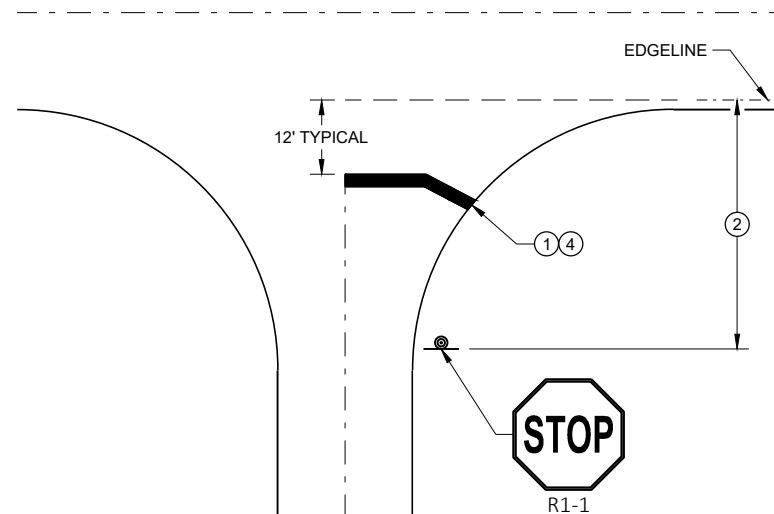
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR
SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

GENERAL NOTES

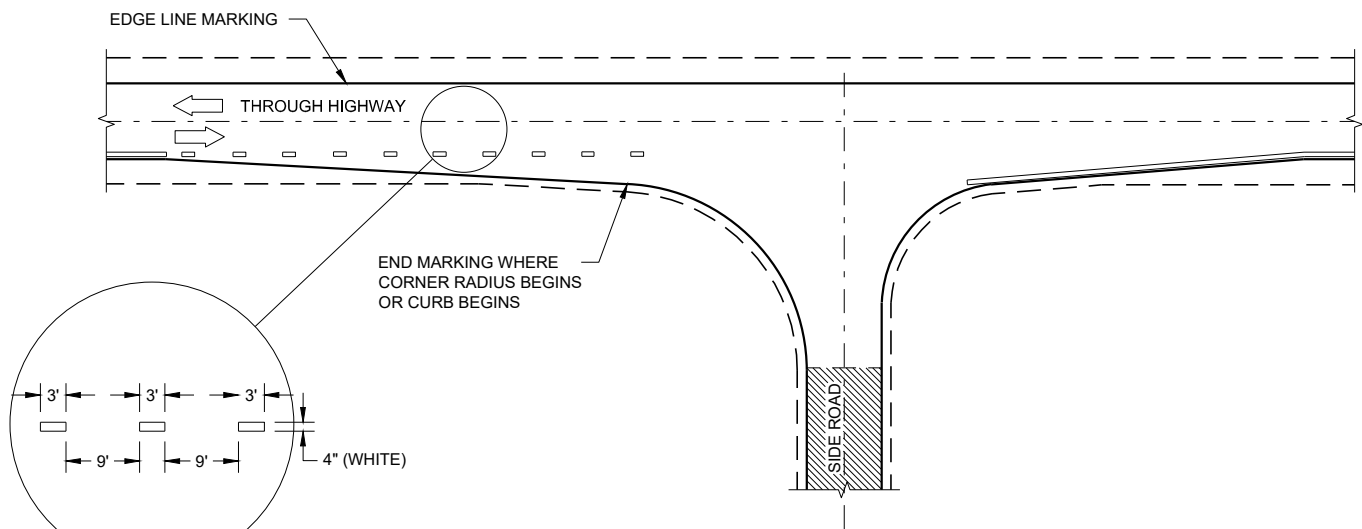
STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGE LINE LOCATION.

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.

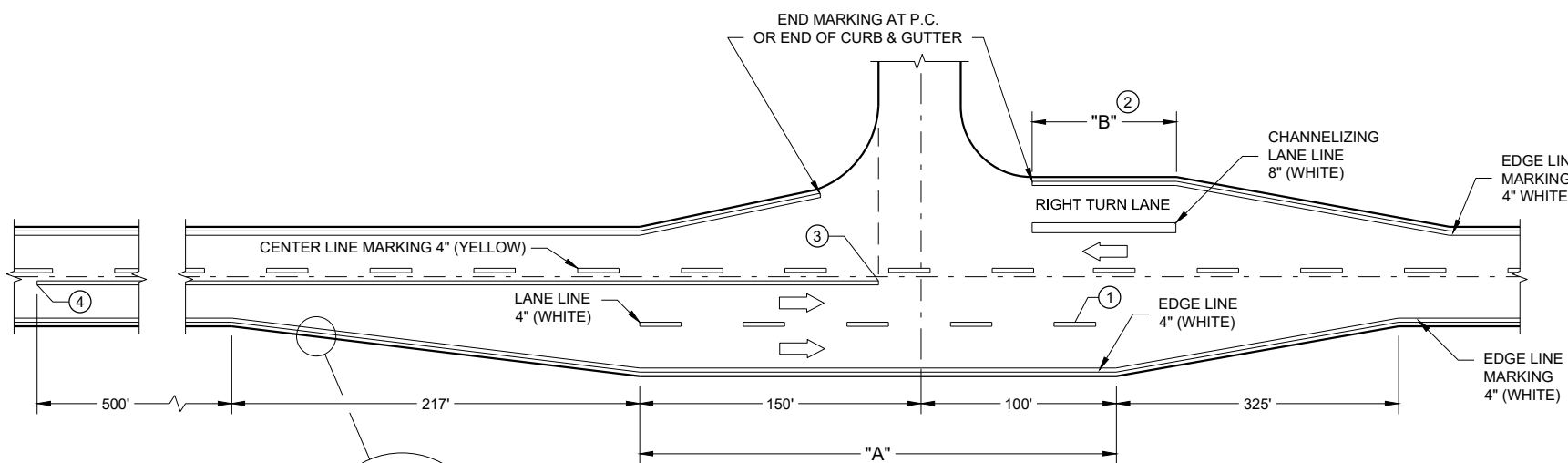
STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER
FHWA



MINOR INTERSECTION



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

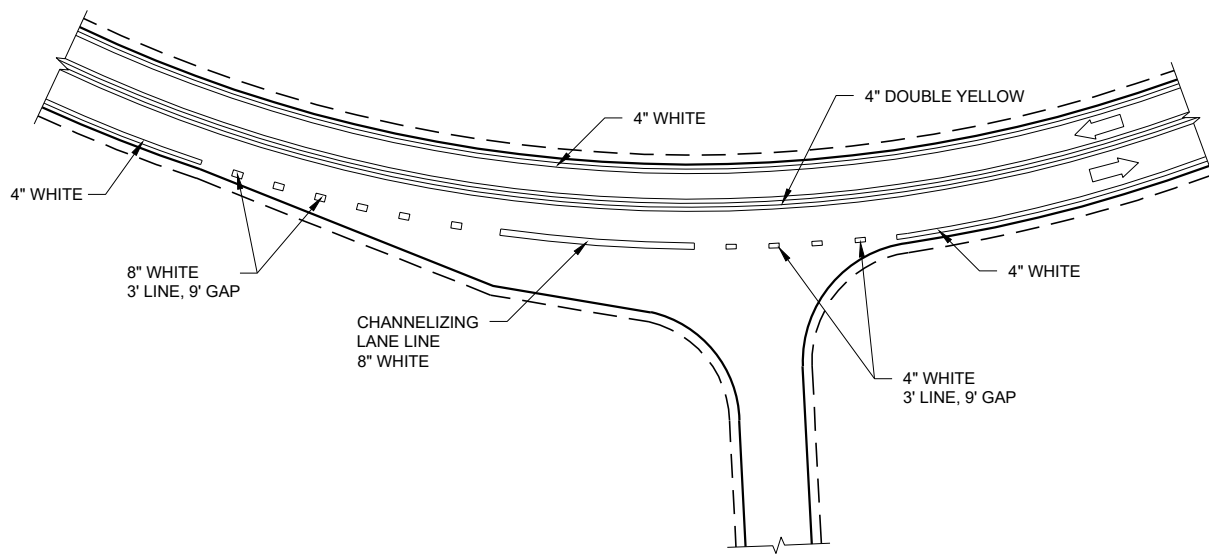
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

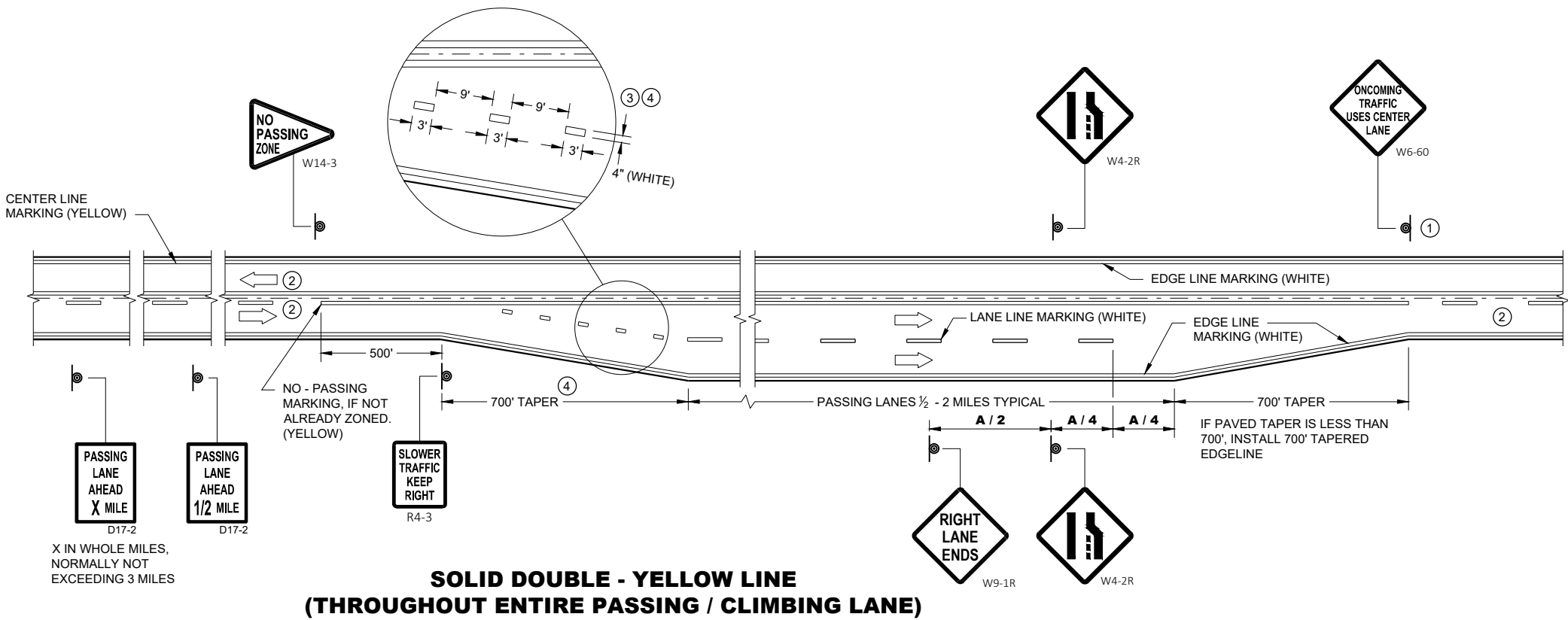
➡ DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE


PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



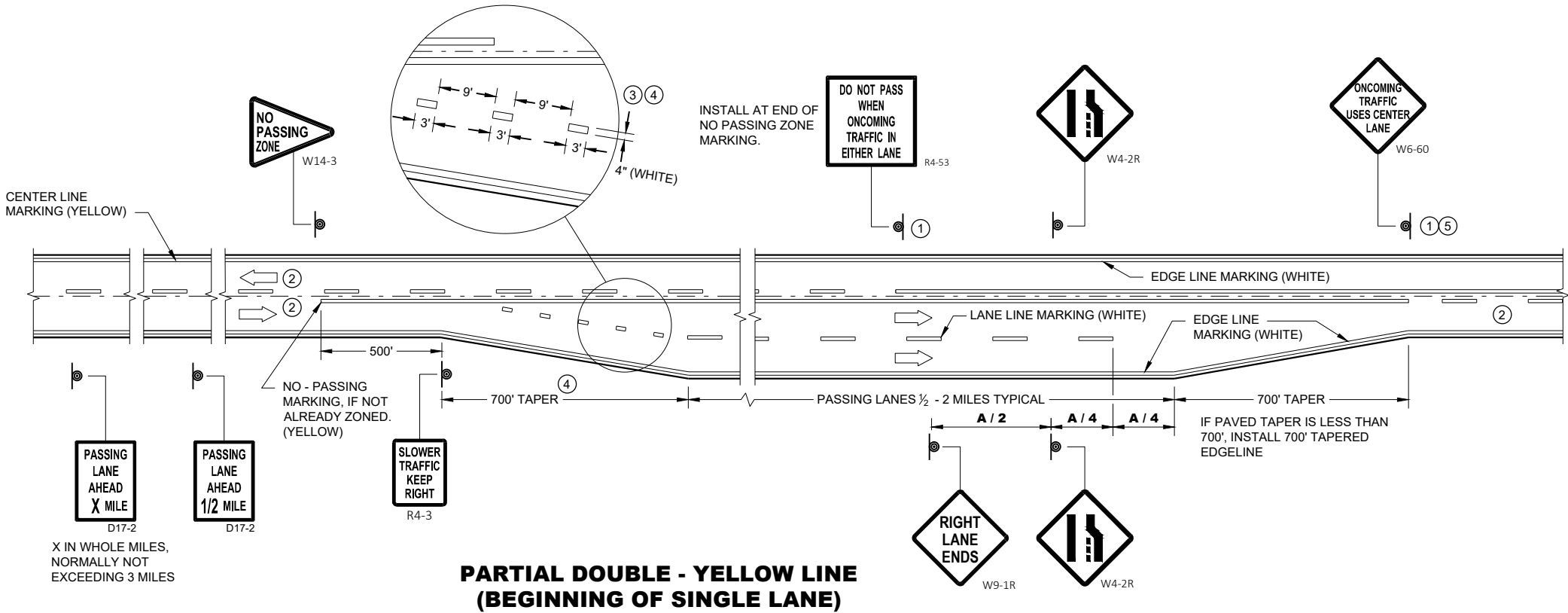
GENERAL NOTES

- 1 SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- 2 THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- 3 THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE, 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- 4 WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING / CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.
- 5 REPEAT EVERY 1 MILE UP UNTIL R4-53.

ARROW SYMBOL () SHOWS DIRECTION OF TRAVEL

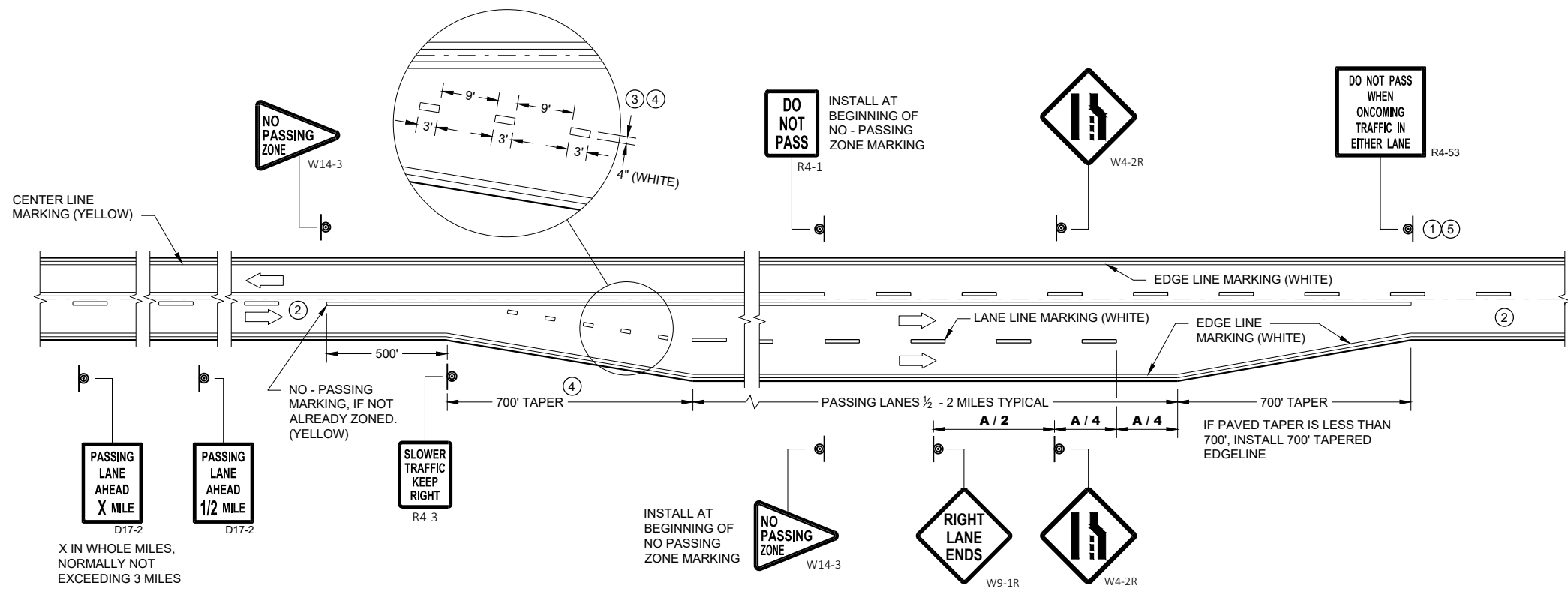
DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	775
50	885
55	990

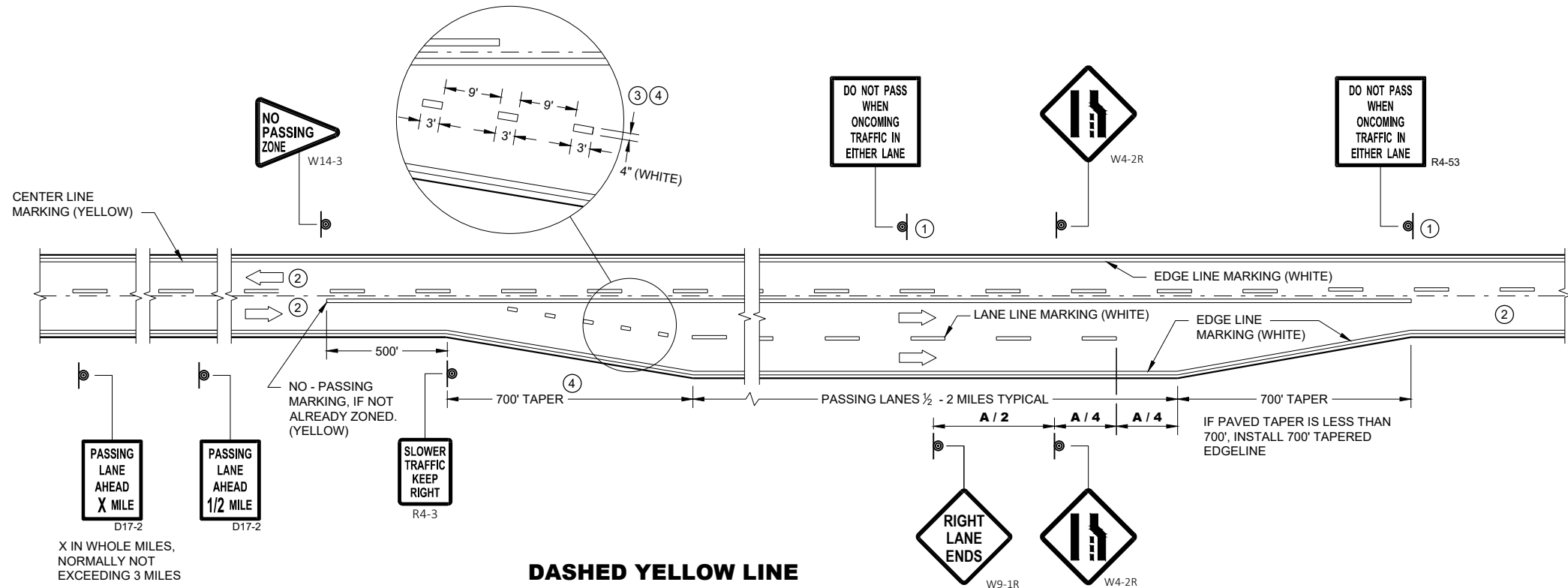


PAVEMENT MARKING & SIGNING
(CLIMBING LANE & PASSING LANE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**SOLID DOUBLE - YELLOW LINE
(END OF SINGLE LANE)**



**DASHED YELLOW LINE
(THROUGHOUT SINGLE LANE)**

GENERAL NOTES

- 1 SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- 2 THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- 3 THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE, 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- 4 WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING / CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.
- 5 REPEAT EVERY ONE MILE UP UNTIL NO PASSING ZONE.

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	775
50	885
55	990

**PAVEMNET MARKING & SIGNING
(CLIMBING LANE & PASSING LANE)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS

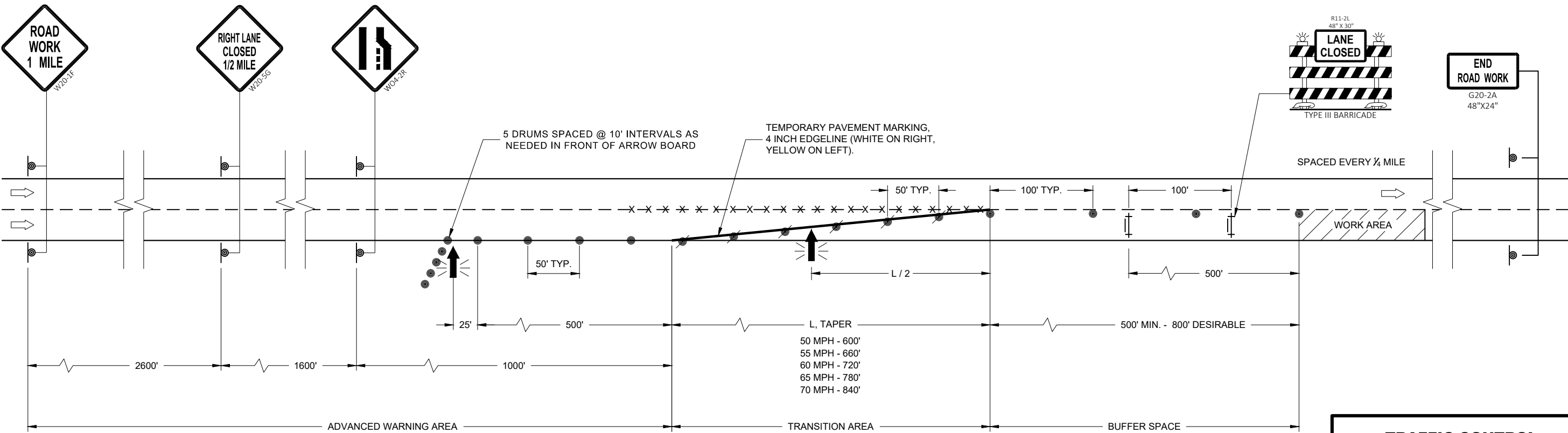
NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC
- WORK AREA
- FLASHING ARROW BOARD

6



6

SDD 15D12 - 08a

SDD 15D12 - 08a

TRAFFIC CONTROL LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

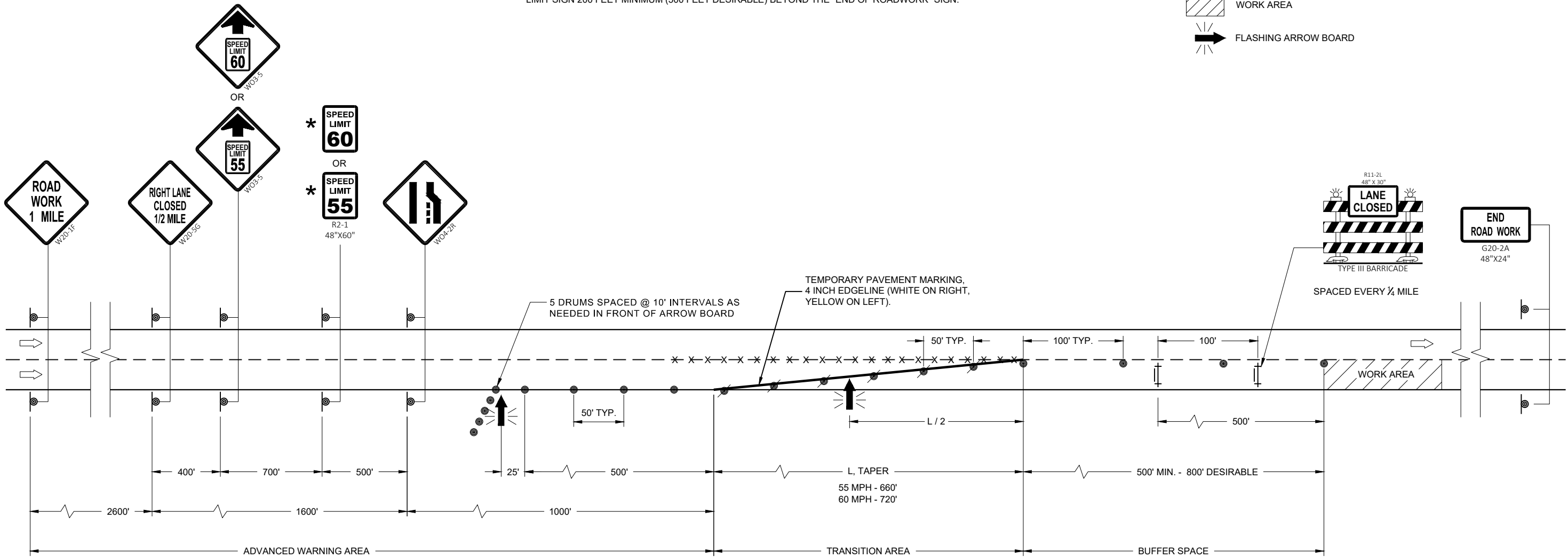
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

* A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES. INCLUDE A RESUME SPEED LIMIT SIGN 200 FEET MINIMUM (500 FEET DESIRABLE) BEYOND THE "END OF ROADWORK" SIGN.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC
- WORK AREA
- FLASHING ARROW BOARD

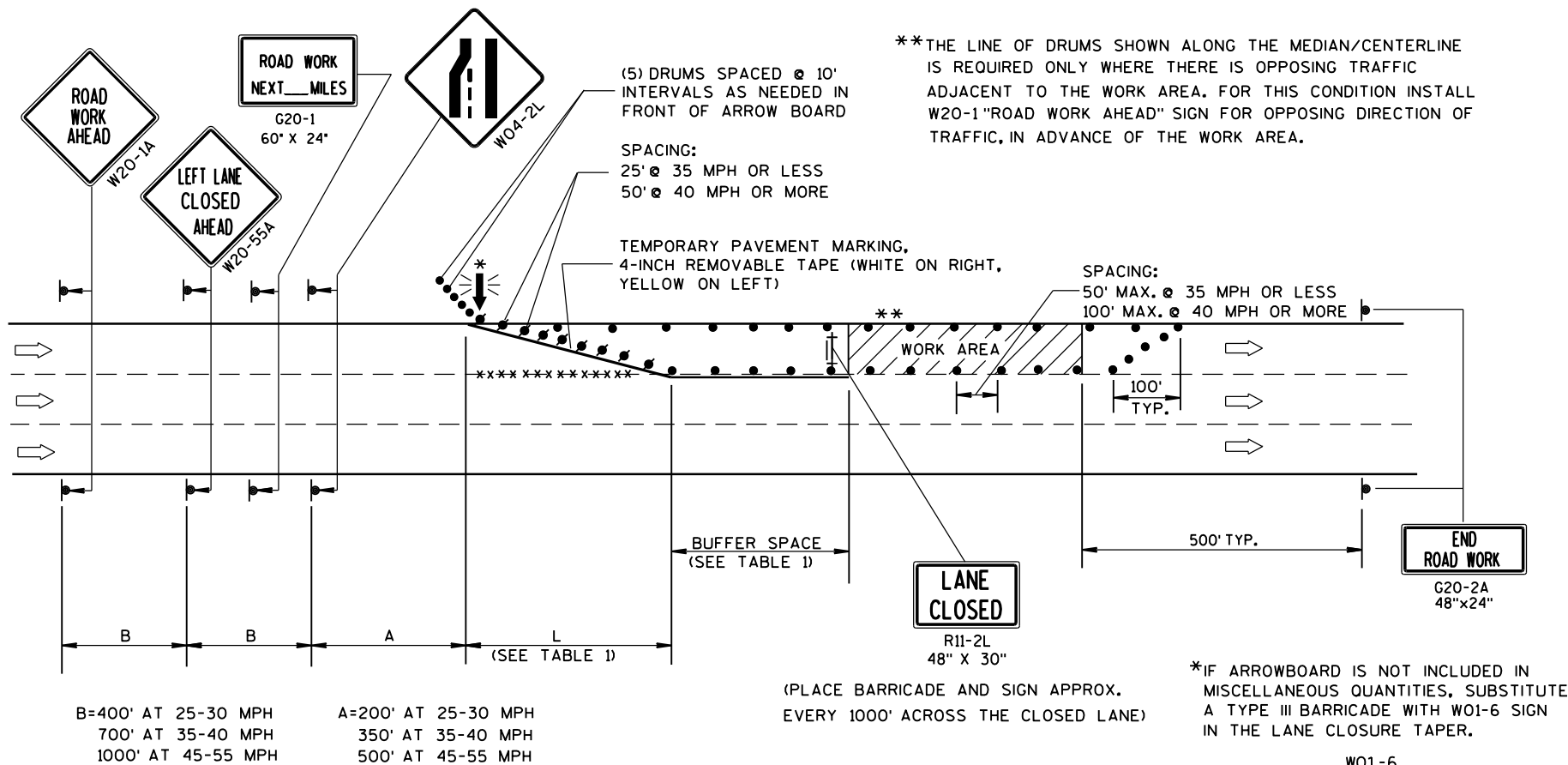


TRAFFIC CONTROL,
LANE CLOSURE,
SPEED REDUCTION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



GENERAL NOTES

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TABLE 1
TAPER AND BUFFER SPACE
FOR 12' LANE WIDTH

S	L	BUFFER SPACE
25	125'	55'
30	180'	85'
35	245'	120'
40	320'	170'
45	540'	220'
50	600'	280'
55	660'	335'

FOR LANE WIDTH OTHER THAN 12':

L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

L = TAPER LENGTH IN FEET

S = NON-CONSTRUCTION SPEED LIMIT (MPH)

W = WIDTH OF LANE CLOSURE

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

TRAFFIC CONTROL,
SINGLE LANE CLOSURE,
NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

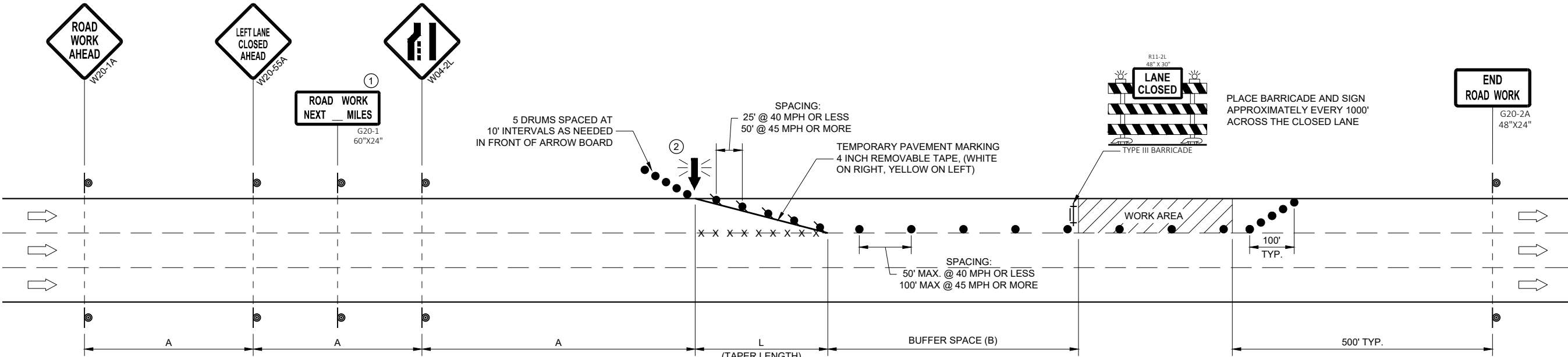
CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

- ① OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- ② WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020
DATE

/S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ⚡➡ FLASHING ARROW BOARD
- ▨ WORK AREA

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

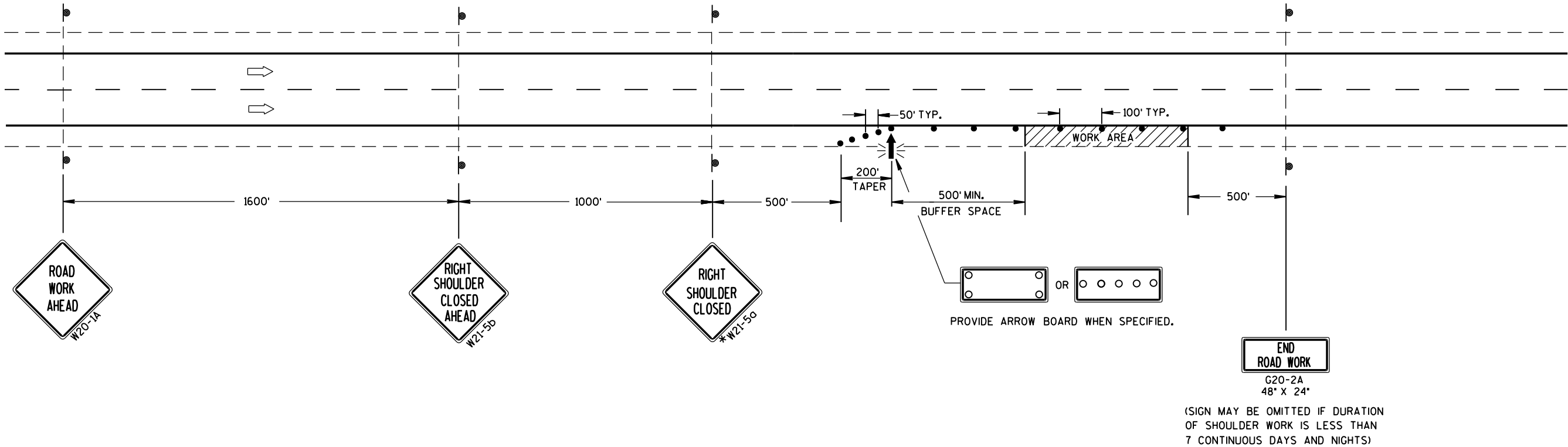
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-5a SIGN MAY BE OMITTED.



TRAFFIC CONTROL
SHOULDER CLOSURE ON DIVIDED
ROADWAY, SPEEDS GREATER
THAN 40 MPH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER

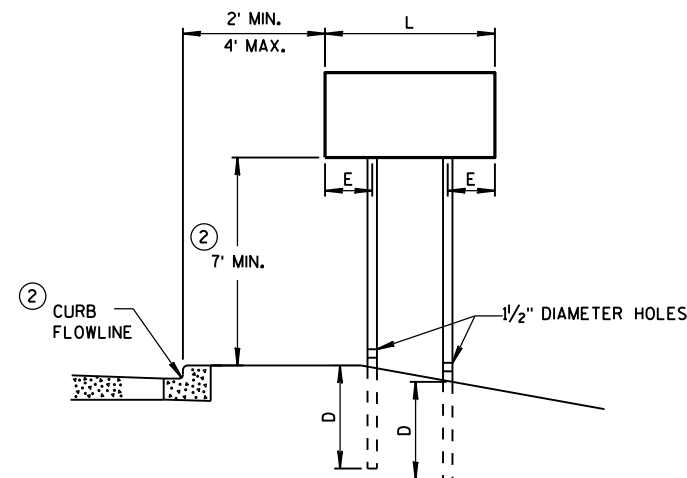
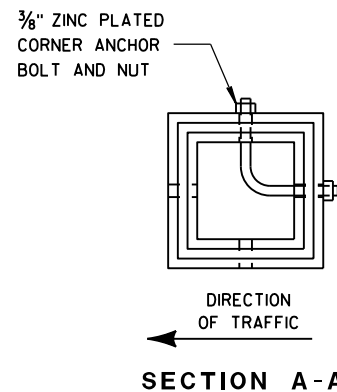


DETAIL OF TUBULAR STEEL SIGN POST

TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).
SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

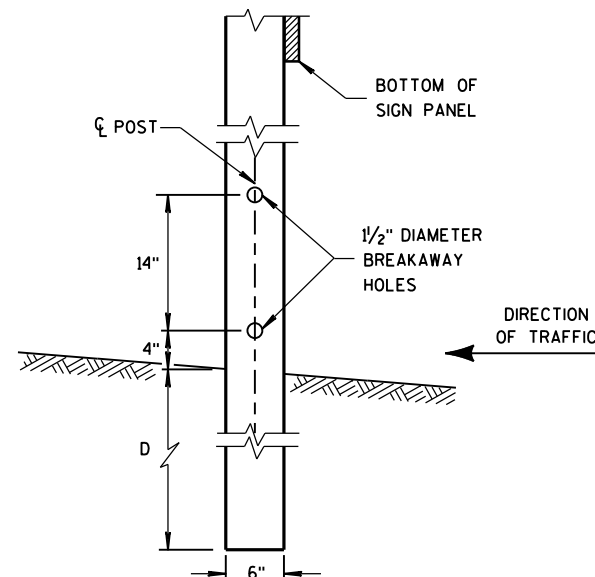


URBAN AREA

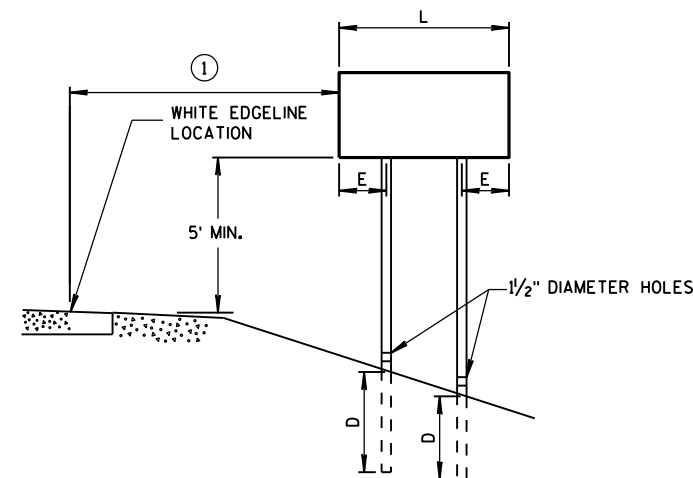
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



4" x 6" WOOD POST MODIFICATION



RURAL AREA

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

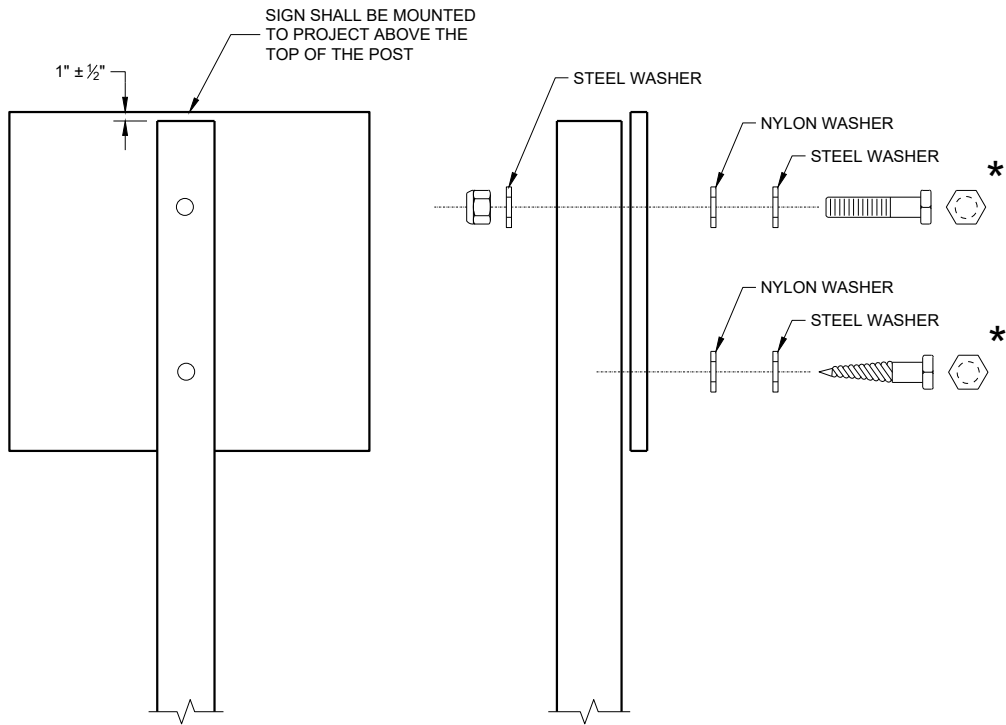
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POST (4" x 6")
LAG SCREWS - ¾" x 3"
MACHINE BOLTS - ⅝" x 6 ½" OR 7" LENGTH W/NUTS

SQUARE STEEL POST (2" x 2")
MACHINE BOLTS - ¾" x 3 ¼" LENGTH W/NUTS
RIVETS - ⅝" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM
BODY/MANDREL O.D. FLANGE 0.720 - 0.765 INCH,
GRIP RANGE 0.042 - 0.375 INCH

WASHERS (ALL POSTS) -
1 ¼" O.D. x ⅜" I.D. x ⅛" STEEL
1 ¼" O.D. x ⅜" I.D. x 0.080 NYLON

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

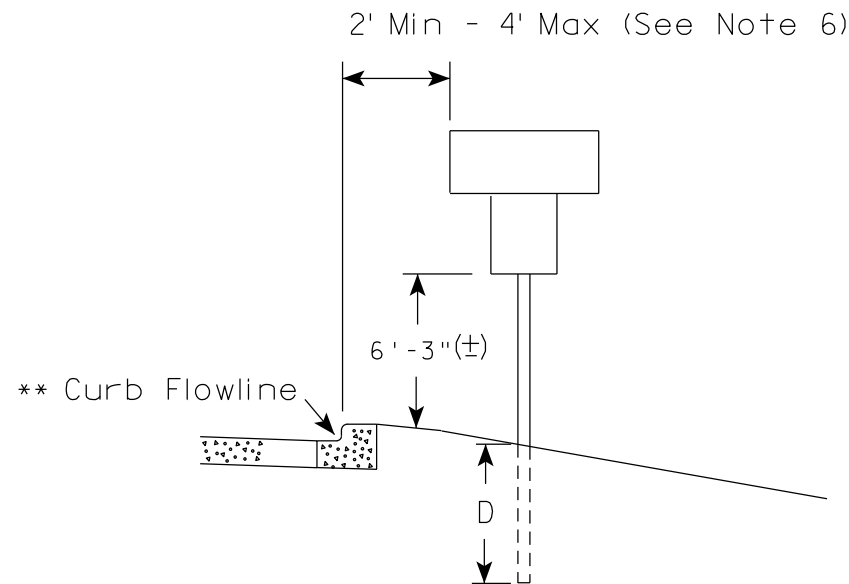
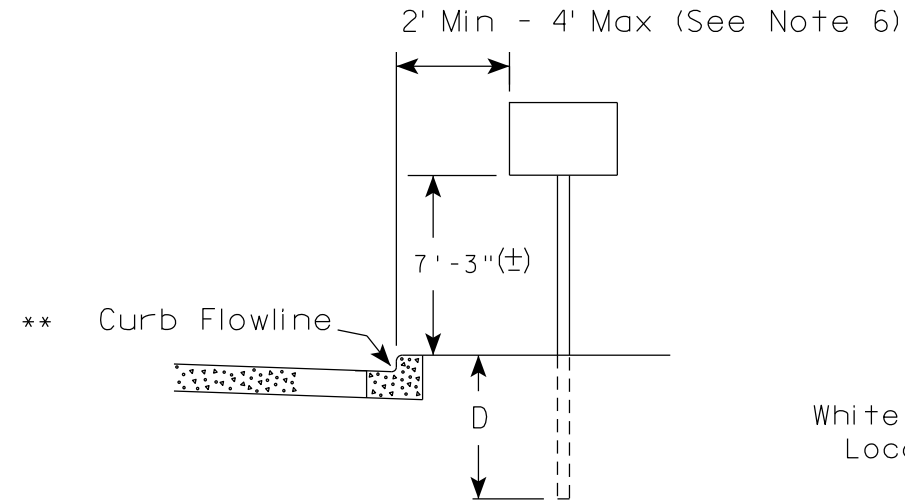
ATTACHMENT OF SIGNS
TO POSTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

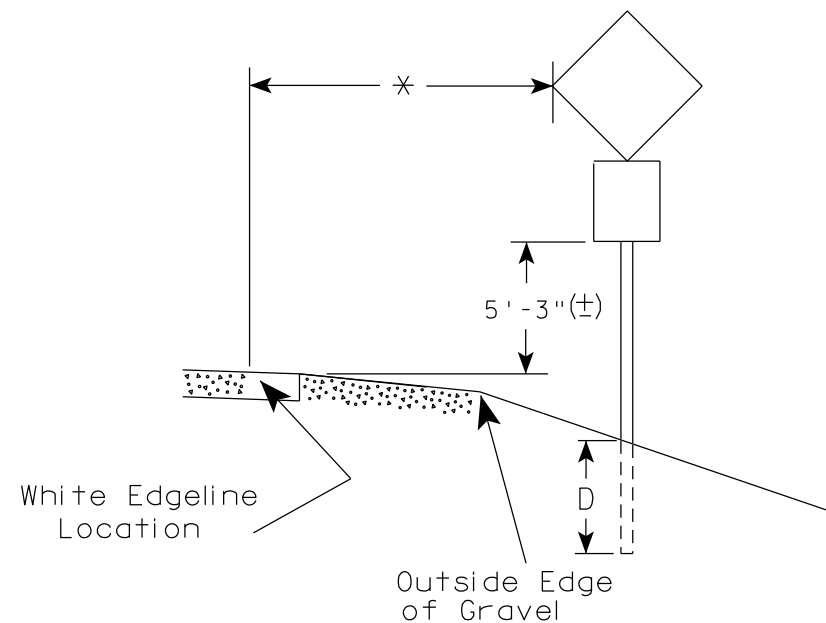
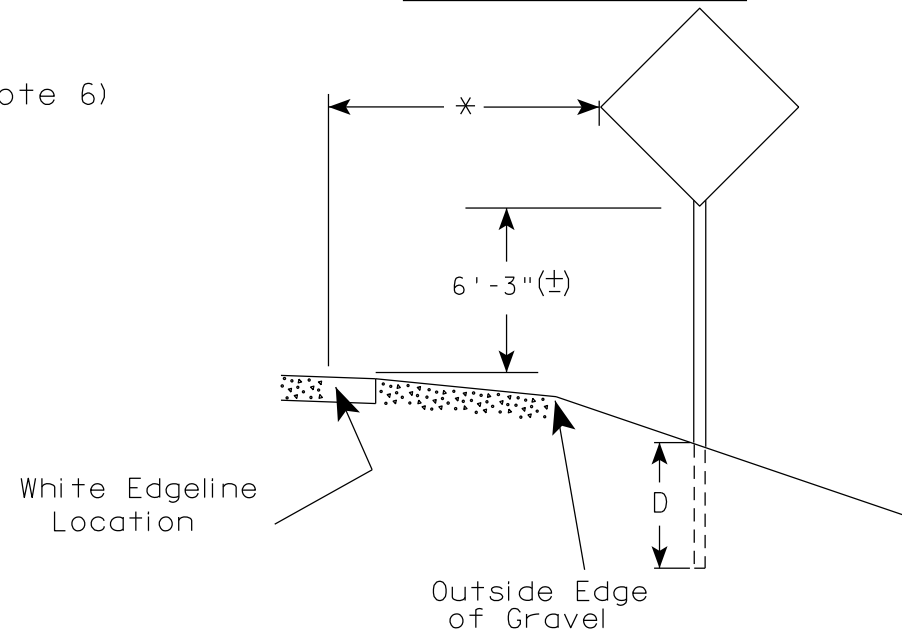
FHWA

URBAN AREA



** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

RURAL AREA (See Note 2)



* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

- Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
- For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
- Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- The (±) tolerance for mounting height is 3 inches.
- Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

**SIGN POST
BOX-OUTS
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

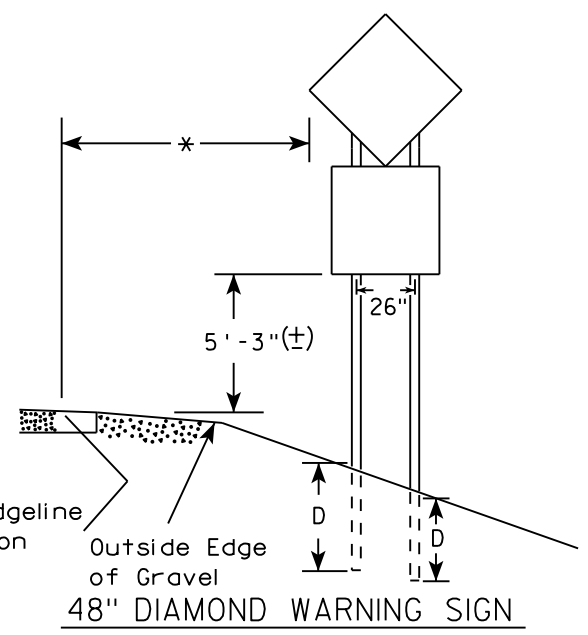
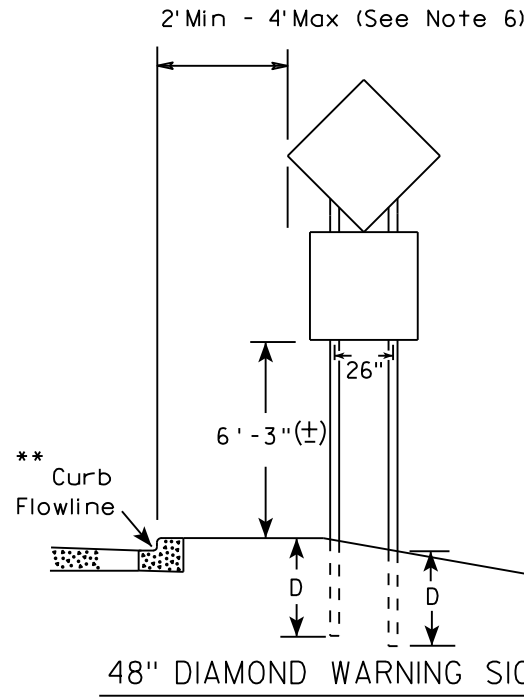
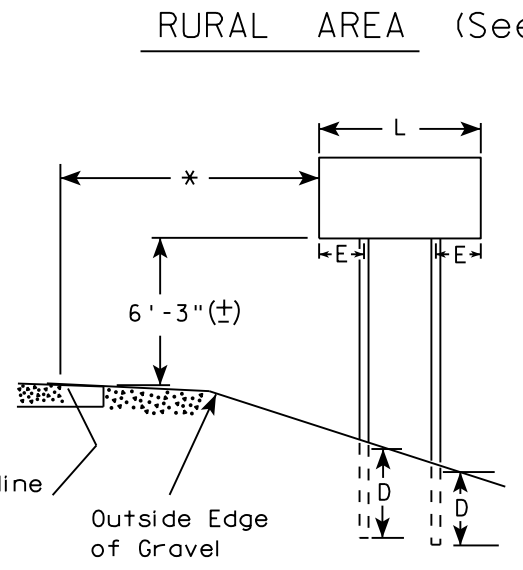
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



- GENERAL NOTES
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. J-Assemblies are considered to be one sign for mounting height.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

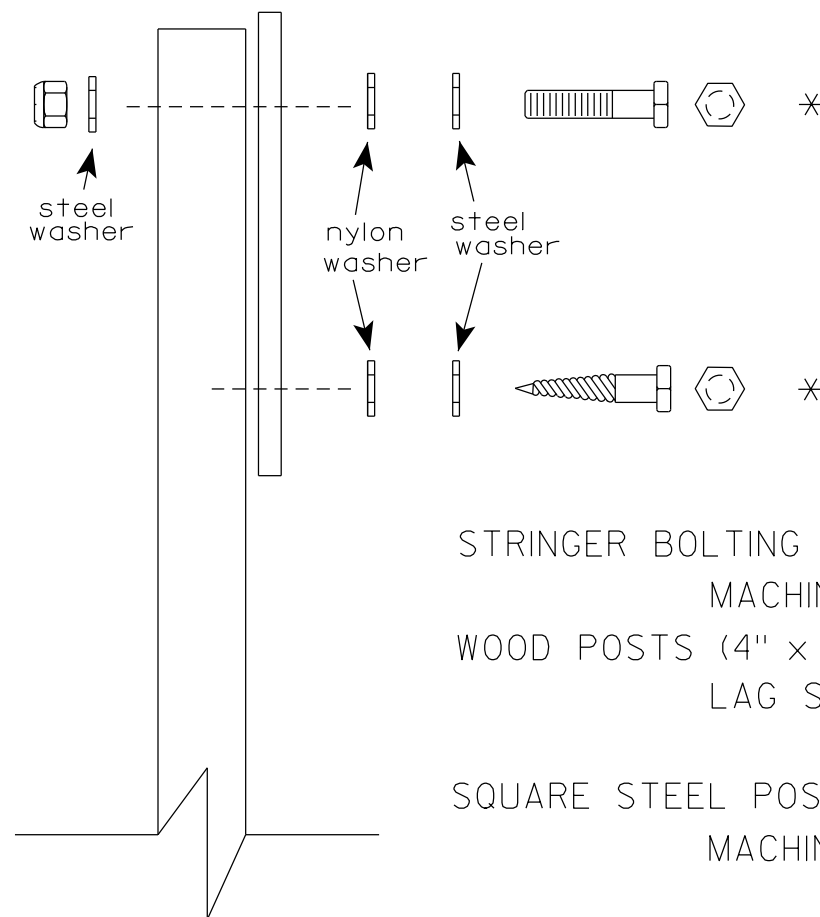
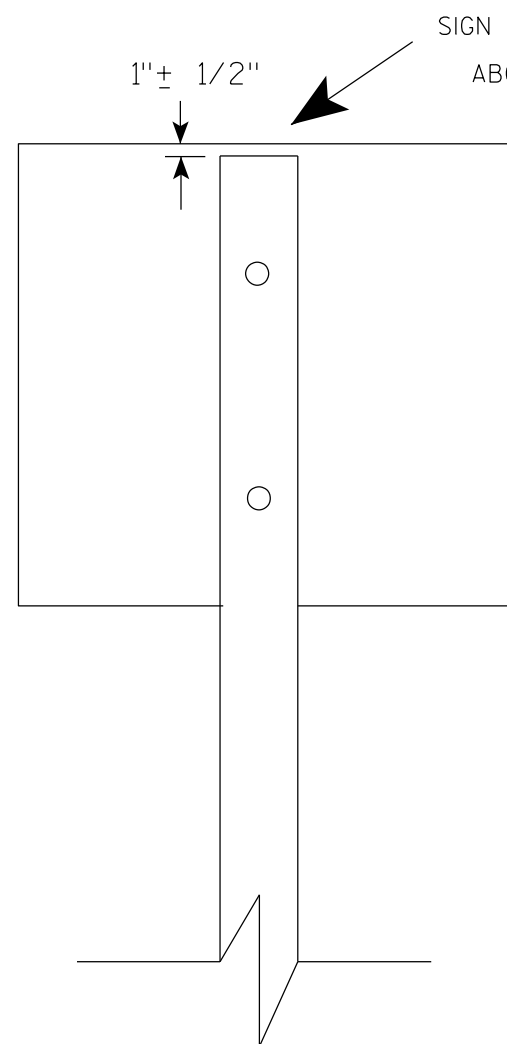
SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 8/21/17	PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

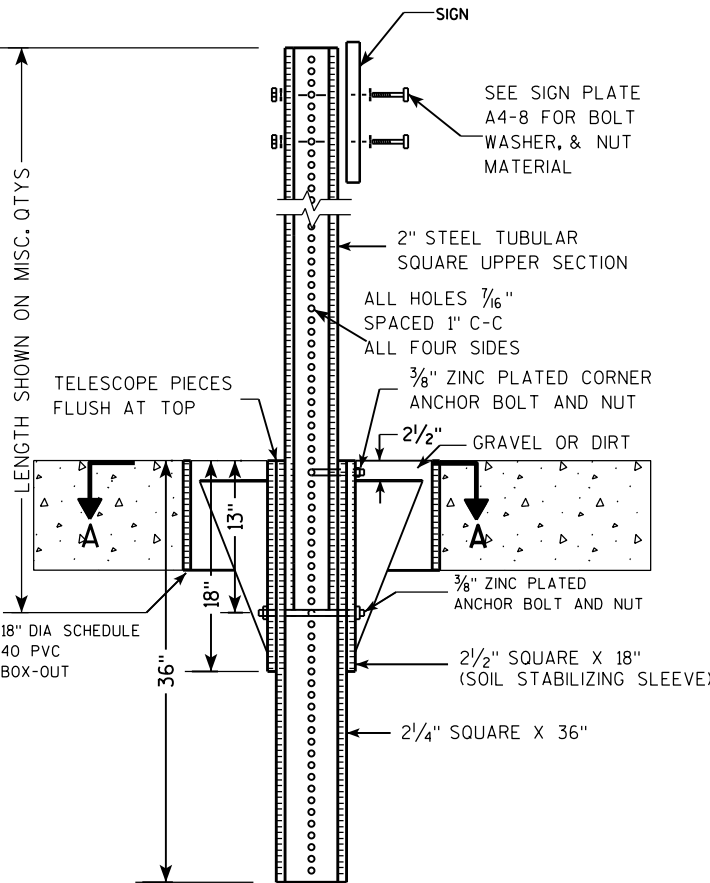
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM



DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

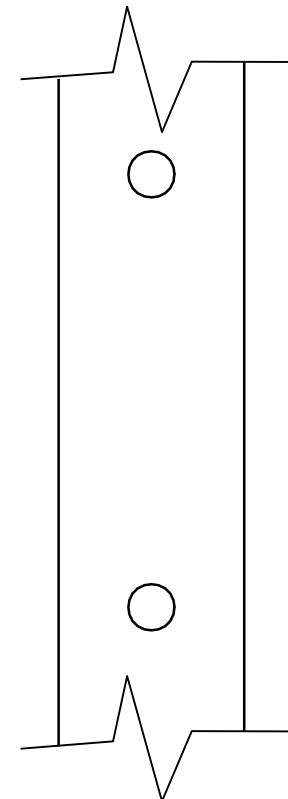
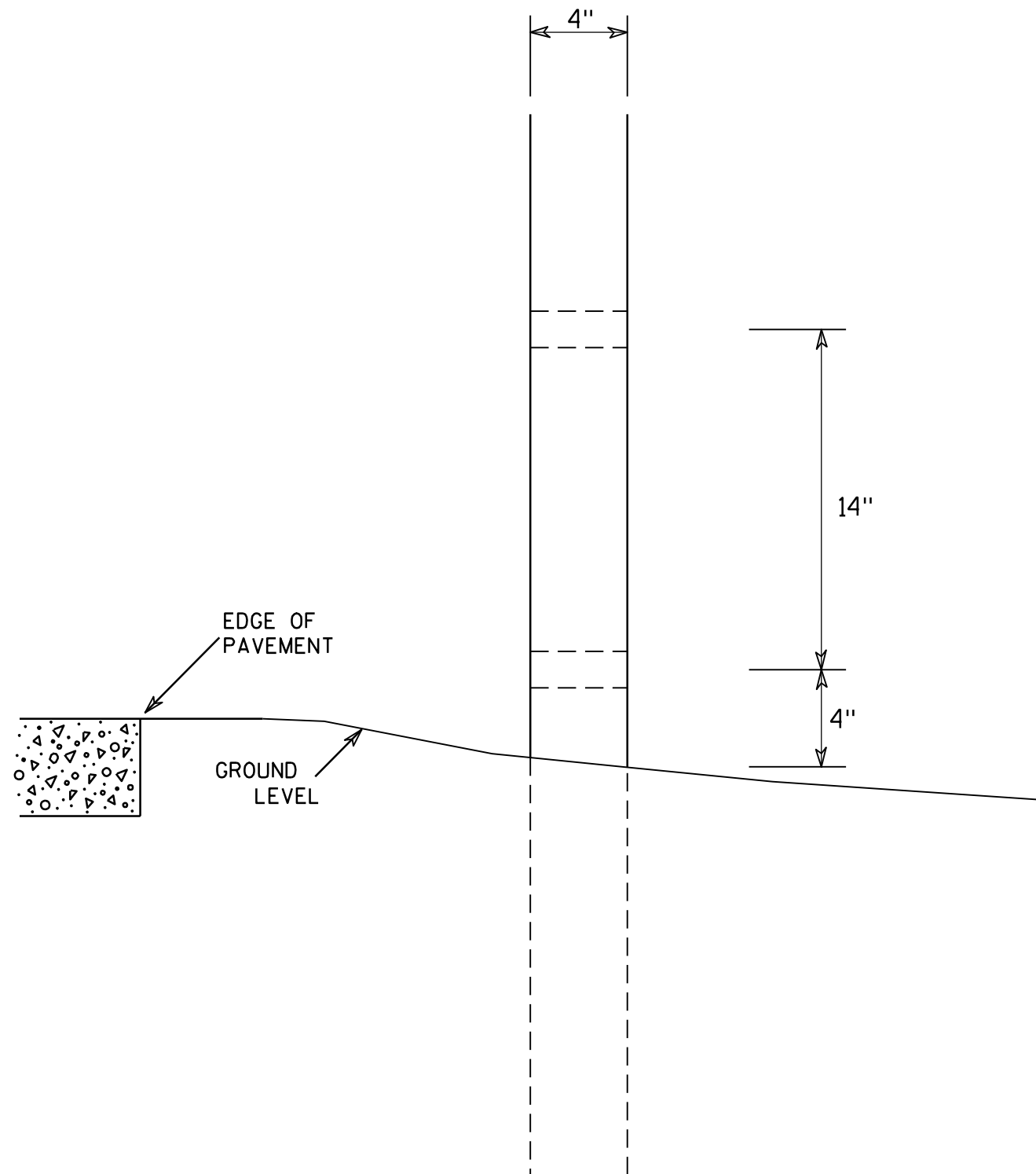
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

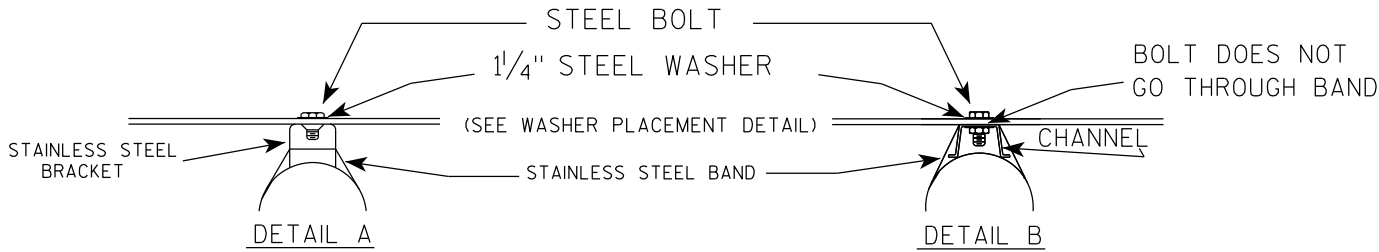
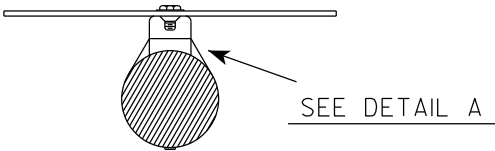
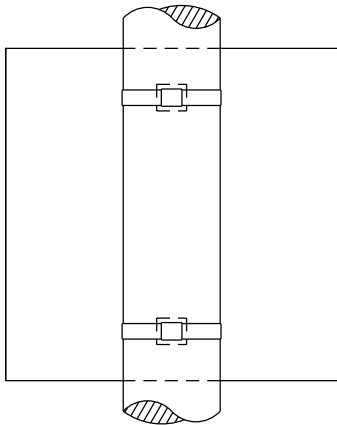
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SHEET NO:

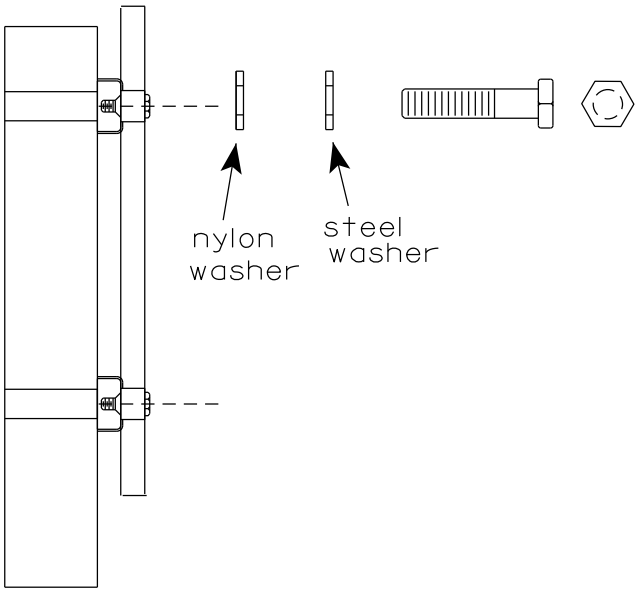
E

BANDING

SINGLE SIGN



WASHER PLACEMENT

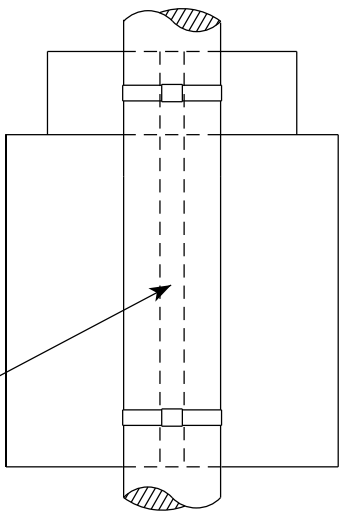


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

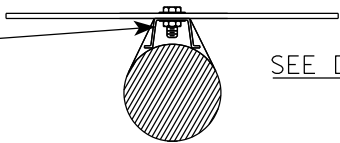
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET

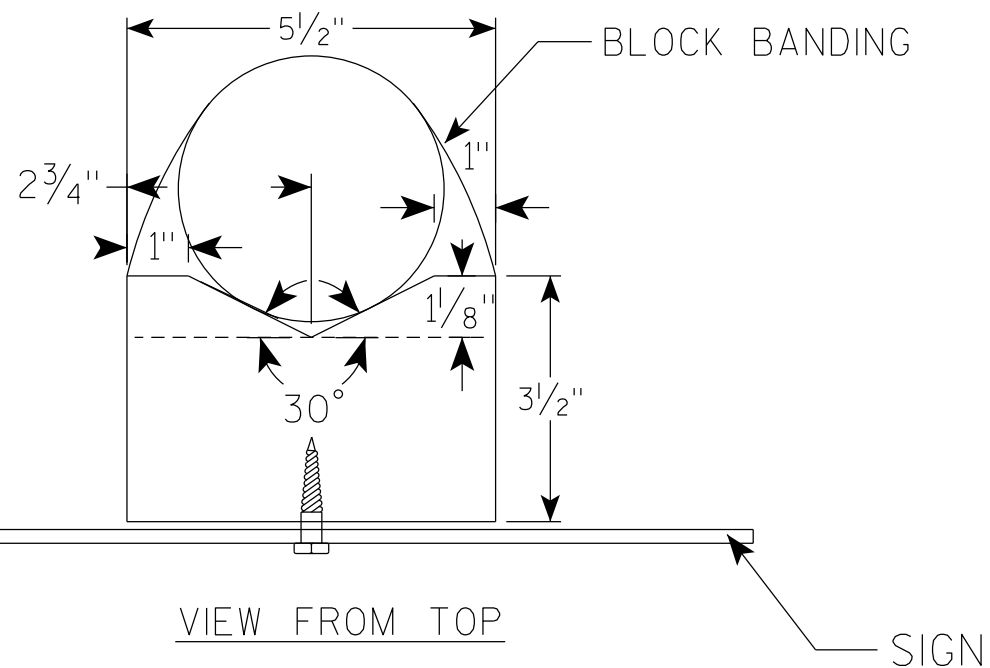
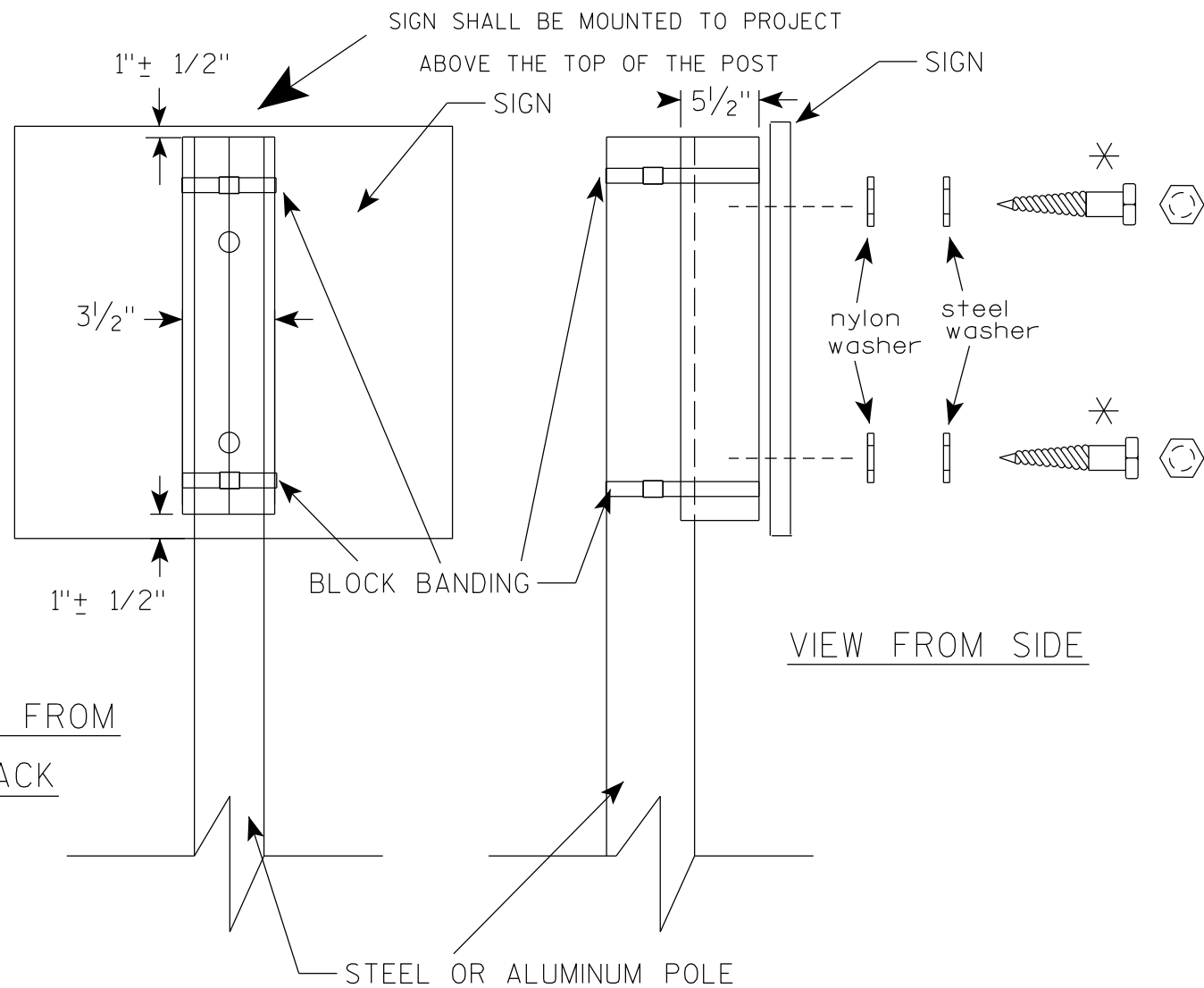


STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/10/19 PLATE NO. A5-9.4

VIEW FROM
BACK



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE $1\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE $1\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

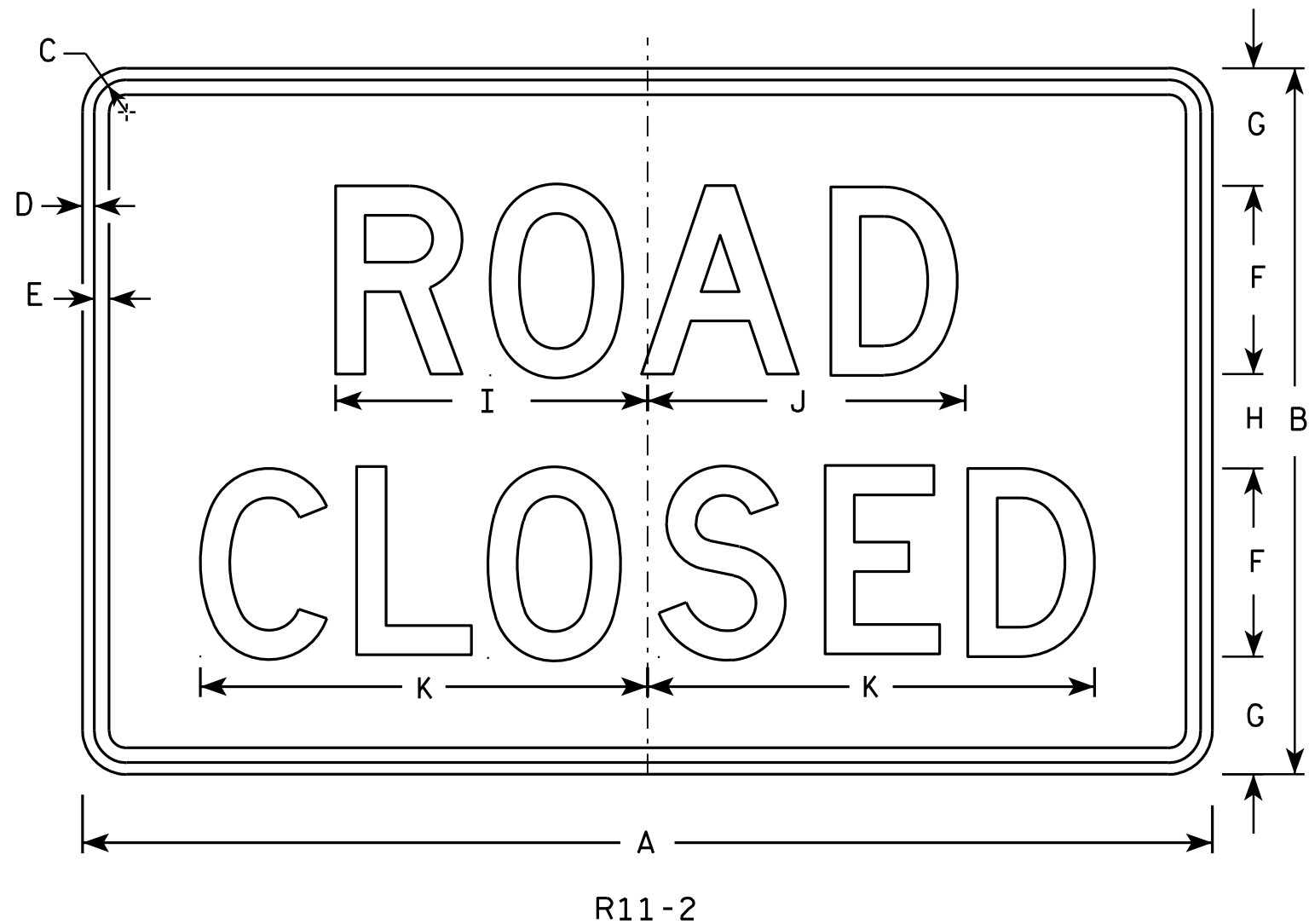
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-10.2

PROJECT NO:

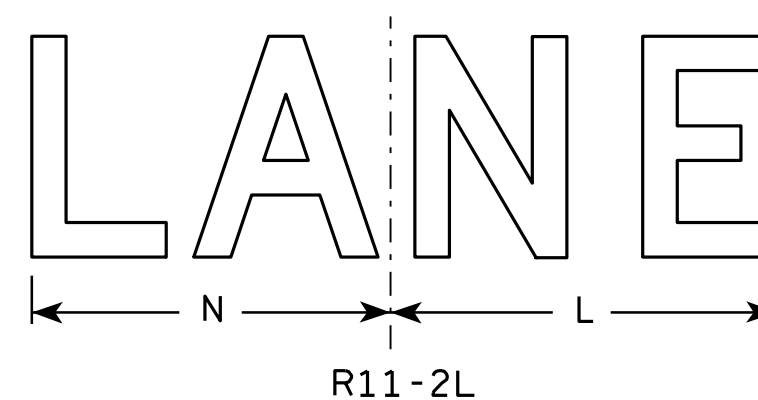
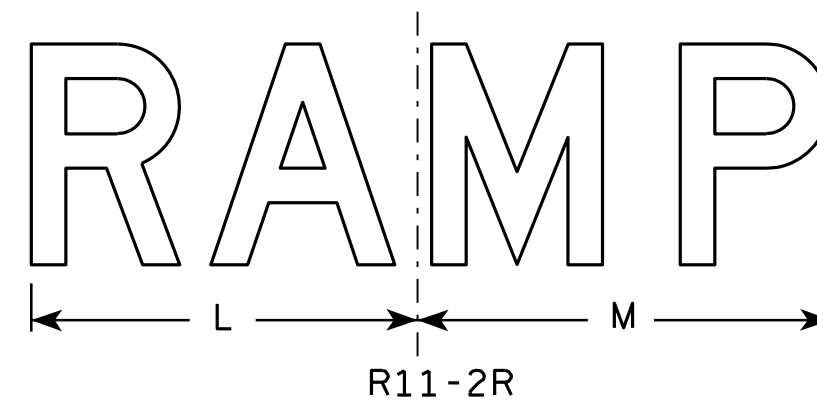
SHEET NO:

E



NOTES

- Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - White
Message - Black
- Message Series - D
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- Modify the message as required.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
2M	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
3	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
4	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
5	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-2.10

PROJECT NO:

HWY:

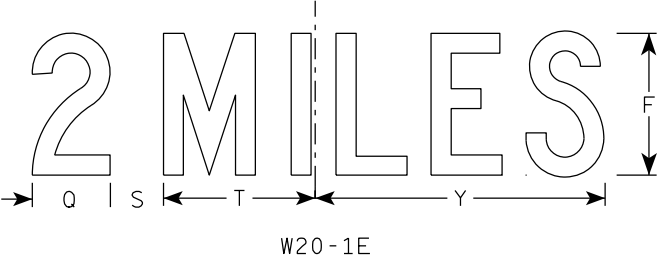
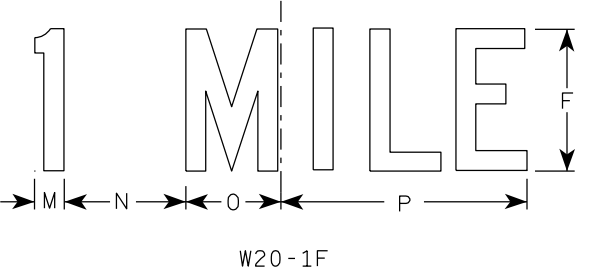
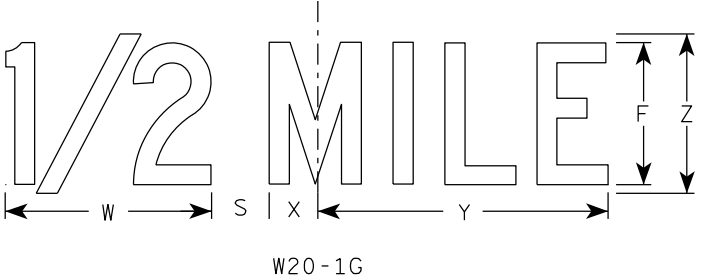
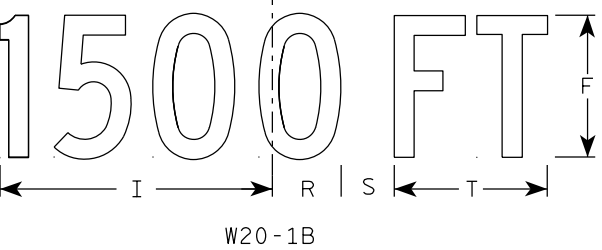
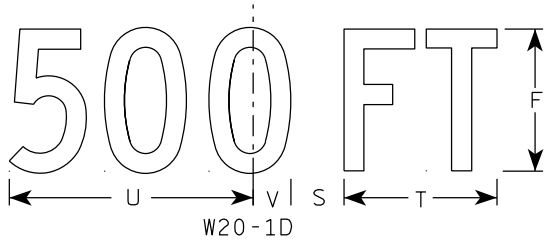
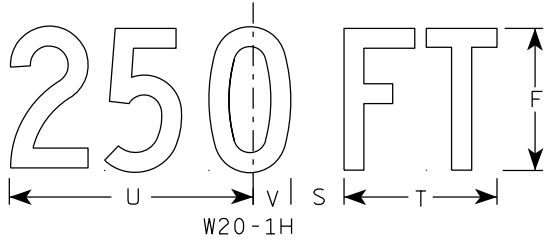
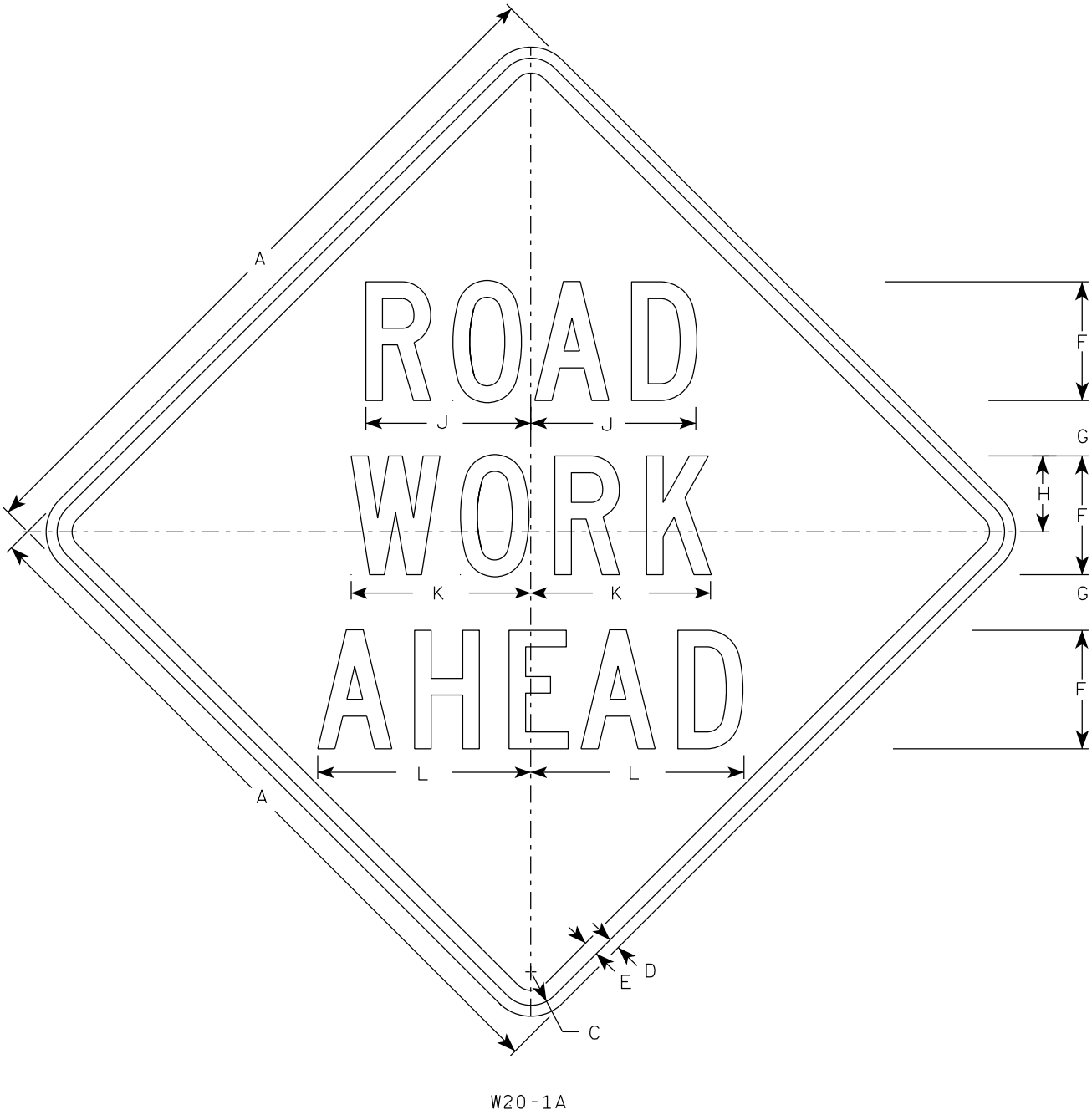
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	A _{req} sq. ft.
1	36		1 5/8	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

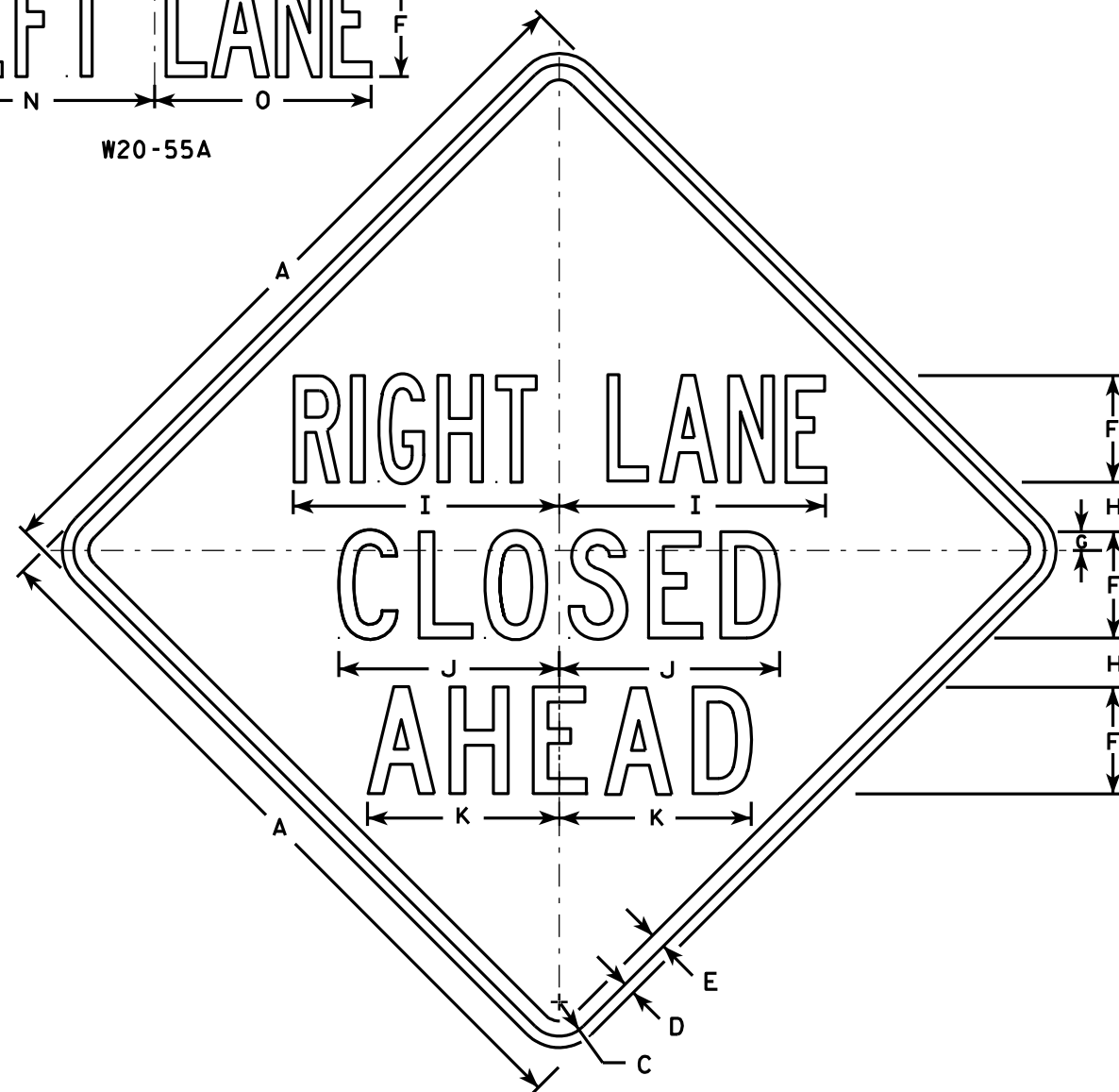
DATE 3/25/2020 PLATE NO. W20-1.11

CENTER LANE

W20-56A

LEFT LANE

W20-55A



W20-5A

500 FT

W20-5D

1000 FT

W20-5C

1500 FT

W20-5B

1/2 MILE

W20-5G

1 MILE

W20-5F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. "----- LANE" is Series B.
All other copy is Series C.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	7/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 7/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

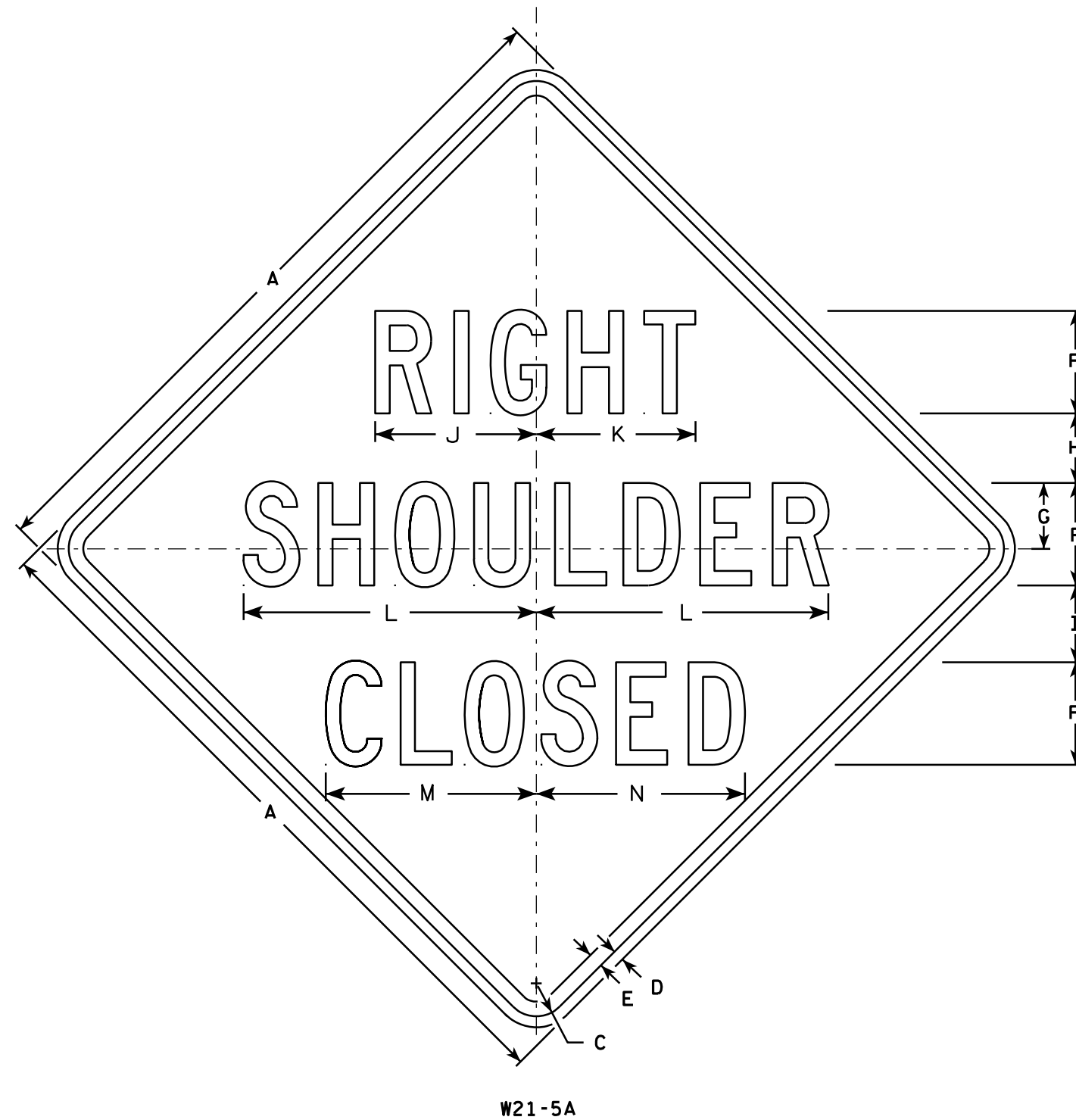
E

STANDARD SIGN
W20-5A, B, C, D, F & G

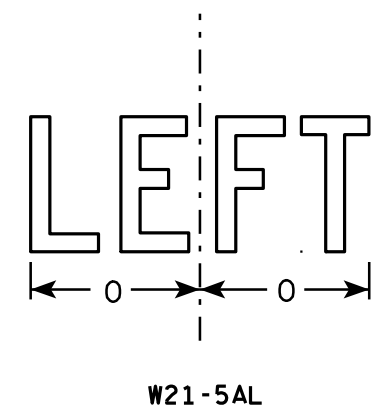
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-5.11



- NOTES**
1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
 2. Color:
Background - Orange
Message - Black
 3. Message Series - D
 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	4	7 7/8	7 3/4	14 1/4	10 1/4	10 1/8	6 1/4												9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	5 1/4	11	10 7/8	20	14 3/8	14 1/4	8 3/4												16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	5 1/4	11	10 7/8	20	14 3/8	14 1/4	8 3/4												16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	5 1/4	11	10 7/8	20	14 3/8	14 1/4	8 3/4												16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	5 1/4	11	10 7/8	20	14 3/8	14 1/4	8 3/4												16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	5 1/4	11	10 7/8	20	14 3/8	14 1/4	8 3/4												16.0

STANDARD SIGN
W21-5A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/21/11 PLATE NO. W21-5A.3

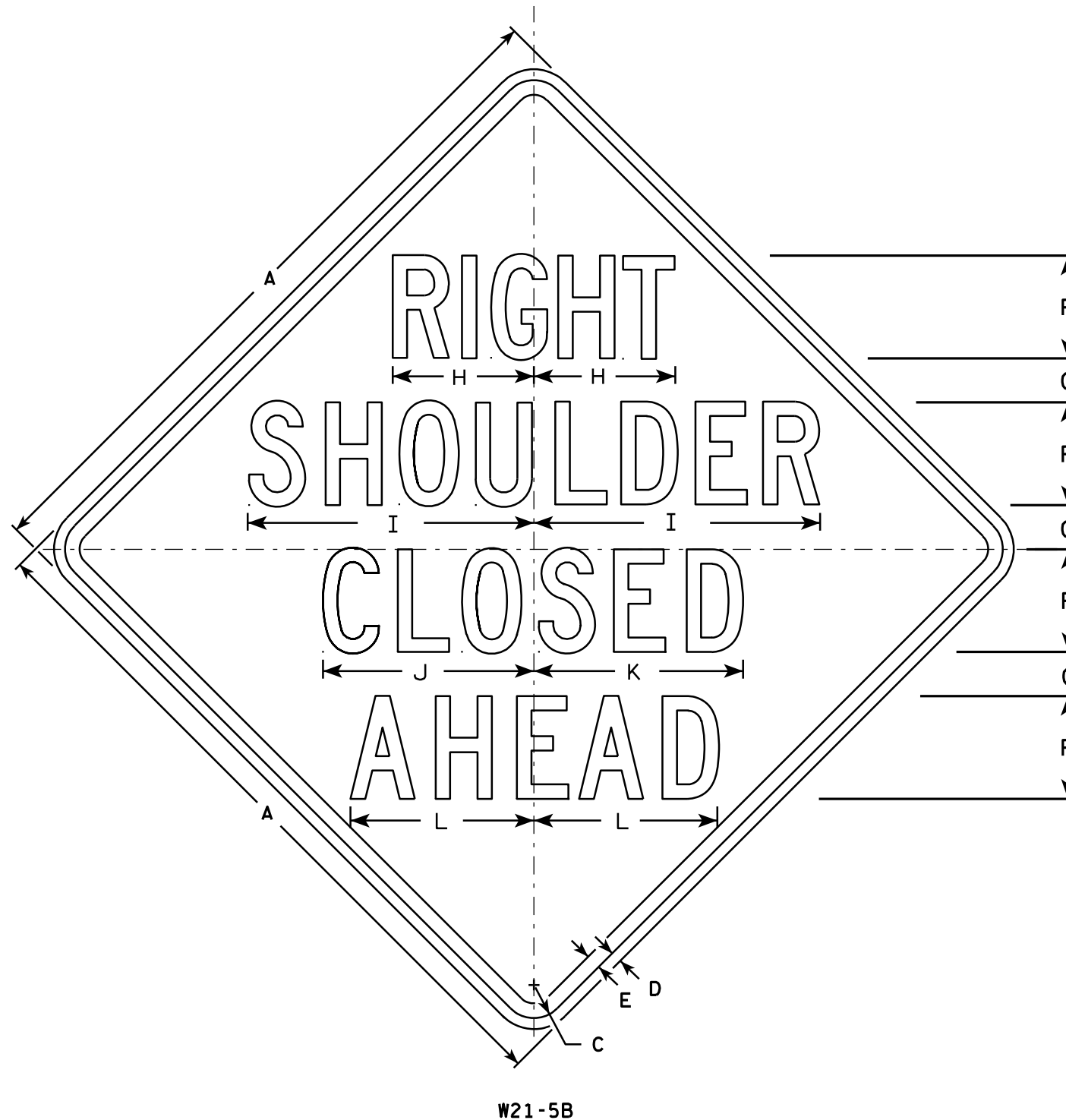
PROJECT NO:

HWY:

COUNTY:

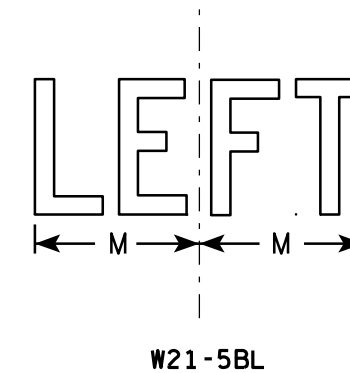
SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 1/4	7 3/8	14 1/4	10 1/4	10 1/8	8 7/8	6 1/4														9.0
2S	48		2 1/4	3/4	1	7	3	9 5/8	19 1/2	14 3/8	14 1/4	12 1/2	8 1/2														16.0
2M	48		2 1/4	3/4	1	7	3	9 5/8	19 1/2	14 3/8	14 1/4	12 1/2	8 1/2														16.0
3	48		2 1/4	3/4	1	7	3	9 5/8	19 1/2	14 3/8	14 1/4	12 1/2	8 1/2														16.0
4	48		2 1/4	3/4	1	7	3	9 5/8	19 1/2	14 3/8	14 1/4	12 1/2	8 1/2														16.0
5	48		2 1/4	3/4	1	7	3	9 5/8	19 1/2	14 3/8	14 1/4	12 1/2	8 1/2														16.0

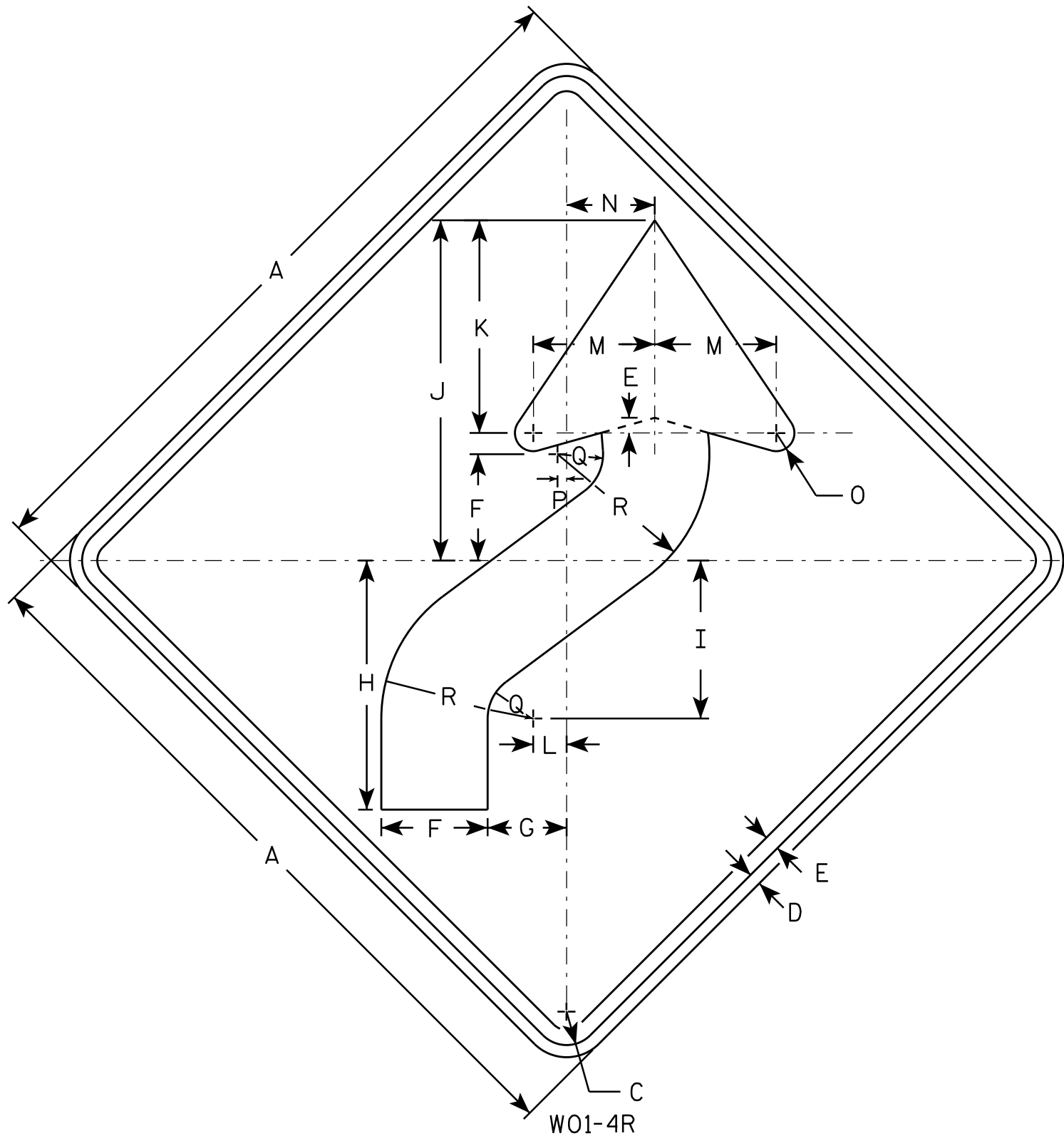
STANDARD SIGN W21-5B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/21/11 PLATE NO. W21-5B.3

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN W01-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-4.1

PROJECT NO:

HWY:

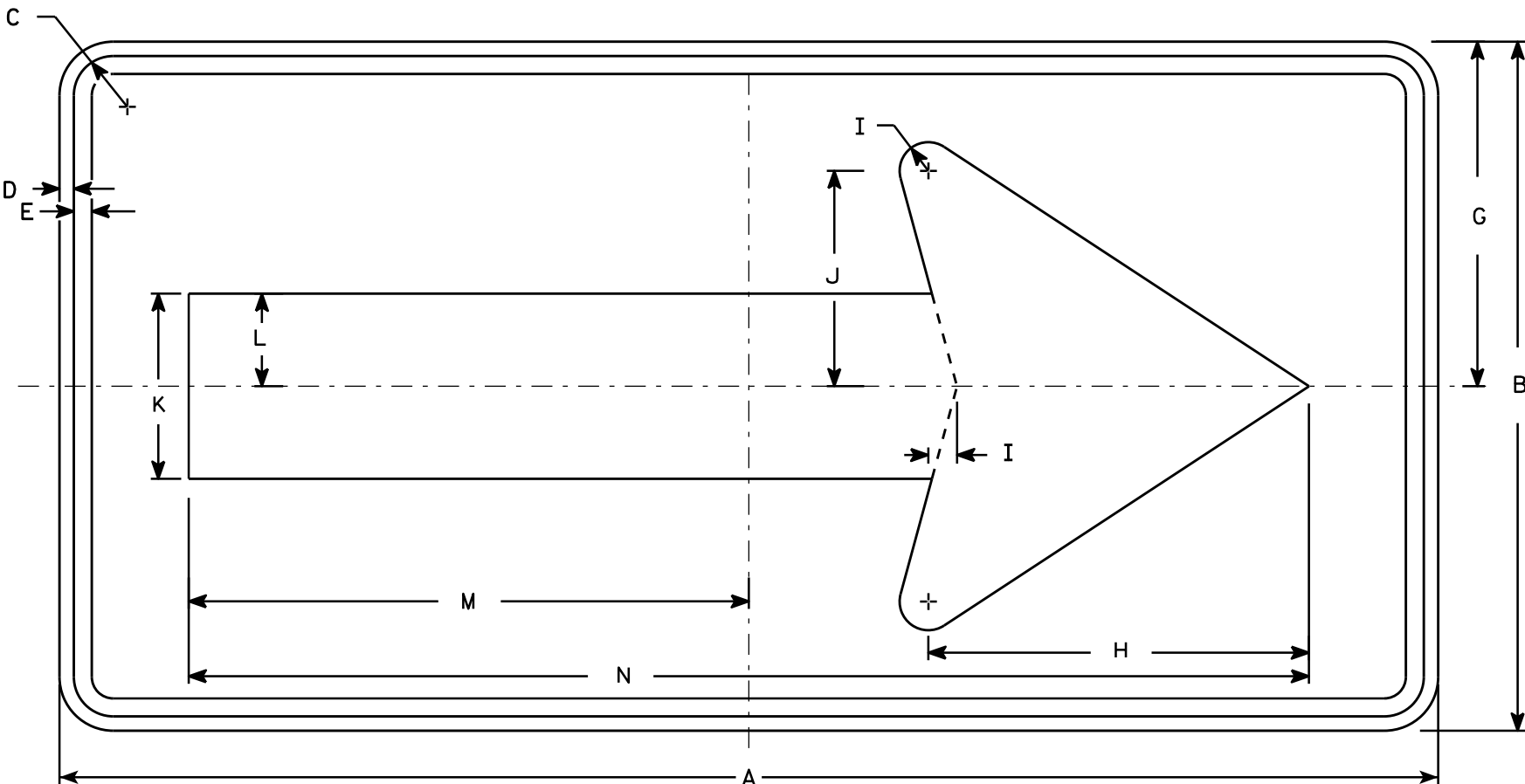
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type F Reflective - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.



W01-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
2M	48	24	1 3⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
3	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
4	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
5	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5

STANDARD SIGN

W01-6

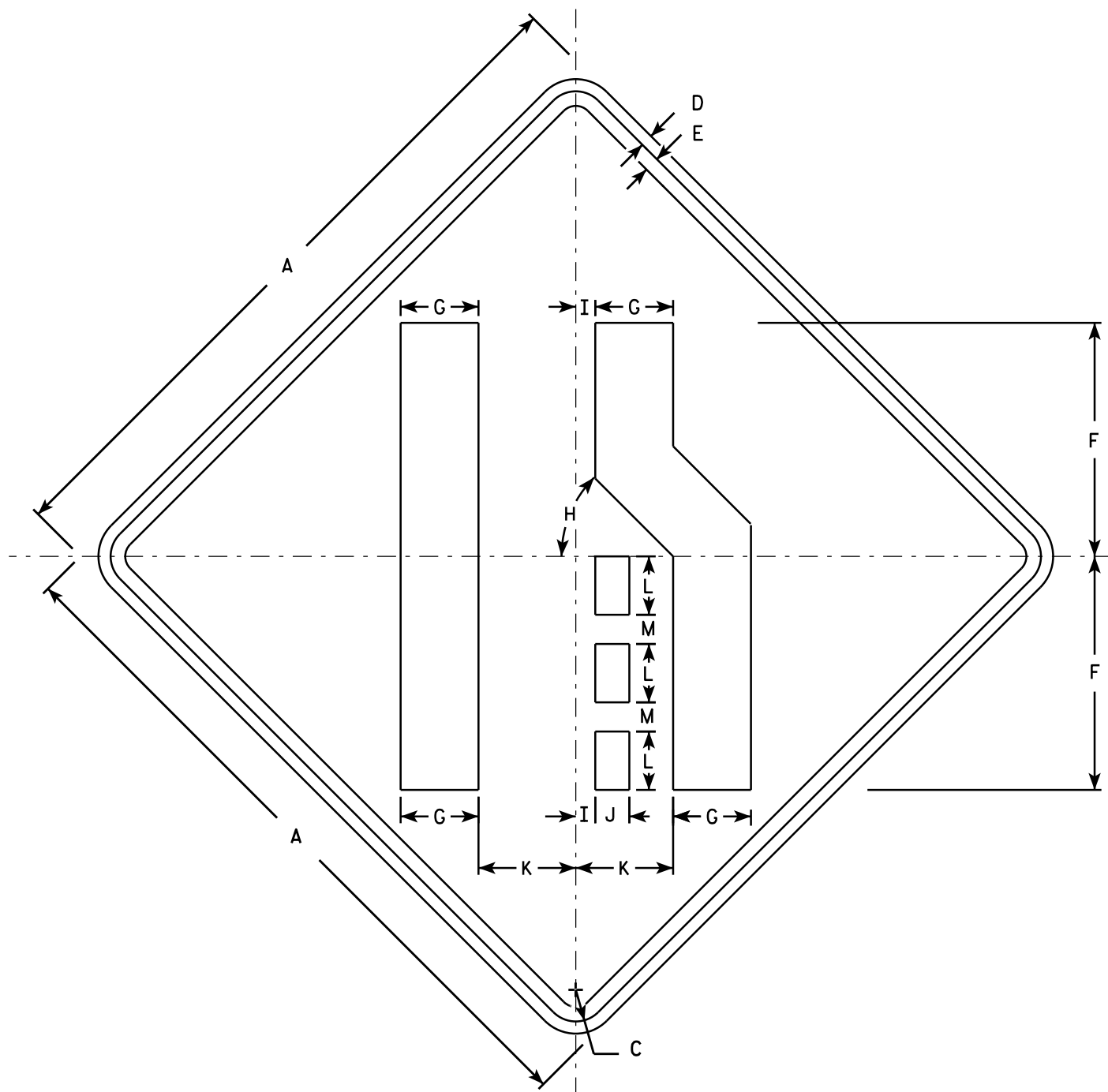
WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

DATE 11/18/13

PLATE NO. W01-6.1



W04-2R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W04-2L is the same as W04-2R except the symbol is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
2M	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
3	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
4	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0

STANDARD SIGN
W04-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
For State Traffic Engineer

DATE 11/20/13 PLATE NO. W04-2.1

PROJECT NO:

SHEET NO:

E



Wisconsin Department of Transportation

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through innovation and exceptional service.

<http://www.dot.wisconsin.gov>