LAX

JANUARY 2021

PROJECT ID: 5478-00-72

WITH: N/A

COUNTY: VERNON

ORDER OF SHEETS

TOTAL SHEETS = 50

N

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

CTH X - STH 27

(TIMBER COULEE CREEK BR, B-62-0260)

CTH P

VERNON COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5478-00-72	WISC 2021079	1

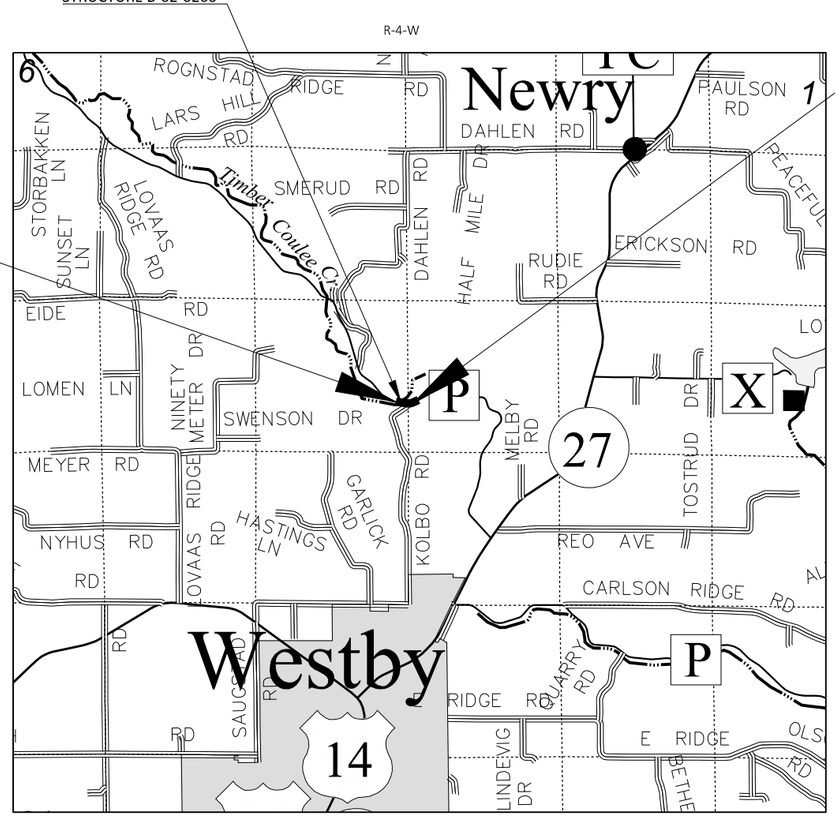
STATE PROJECT NUMBER

5478-00-72

STRUCTURE B-62-0260

BEGIN PROJECT 5478-00-72
STA 102+20
X = 712,246.12
Y = 196,868.66

END PROJECT 5478-00-72
STA 103+45.50



CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	ROCK
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	LABEL
EXISTING RIGHT OF WAY		GRADE ELEVATION	95.36
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		**UTILITIES**	
REFERENCE LINE	300'EB'	ELECTRIC	E
EXISTING CULVERT		FIBER OPTIC	FO
PROPOSED CULVERT (Box or Pipe)		GAS	G
COMBUSTIBLE FLUIDS	CAUTION	SANITARY SEWER	SAN
		STORM SEWER	SS
MARSH AREA		TELEPHONE	T
		WATER	W
		UTILITY PEDESTAL	
WOODED OR SHRUB AREA		POWER POLE	
		TELEPHONE POLE	

DESIGN DESIGNATION			BRIDGE REPLACEMENT
A.A.D.T.	(2021)	=	194
A.A.D.T.	(2041)	=	237
D.H.V.		=	N/A
D.D.		=	50/50
T.		=	9.7%
DESIGN SPEED		=	40 MPH
ESALS		=	29,200

LAYOUT

SCALE 0 1.0 MI

TOTAL NET LENGTH OF CENTERLINE = 0.024 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), VERNON COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ACCEPTED FOR:
COUNTY OF VERNON

DATE: 7-6-20 [Accepting Authority Signature]

ORIGINAL PLANS PREPARED BY

KL Engineering [A] Better Experience

WISCONSIN PROFESSIONAL ENGINEER
CHAD D. HALVERSON
E-34364
MADISON, WI

DATE: 7/10/2020 (Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	KL ENGINEERING
Designer	KL ENGINEERING
Regional Examiner	ALEIGHA BURG
Regional Supervisor	OSCAR I. WINGER

APPROVED FOR THE DEPARTMENT

DATE 7/17/2020 Aleigha Burg, P.E. Digitally signed by Aleigha Burg, P.E. Date: 2020.07.17 10:49:02-05'00' (Signature)

E

STANDARD ABBREVIATIONS

ASPH	ASPHALT
AVG	AVERAGE
BAD	BASE AGGREGATE DENSE
BG	BEAMGUARD
BLDG	BUILDING
BM	BENCH MARK
CMCP	CULVERT METAL CULVERT PIPE
CONC	CONCRETE
CP	CONTROL PIPE
CPCS	CULVERT PIPE CORRUGATED STEEL
D	DEGREE OF CURVE
DISCH	DISCHARGE
EP	EDGE OF PAVEMENT
EXIST	EXISTING
ELEC	ELECTRIC
FO	FIBER OPTIC
HMA	HOT MIX ASPHALT
IV	INVERT
L	LENGTH OF CURVE
LHF	LEFT HAND FORWARD
LT	LEFT
MAX	MAXIMUM
MIN	MINIMUM
NC	NORMAL CROWN
NOR	NORMAL
PAVT	PAVEMENT
PC	POINT OF CURVE
PI	POINT OF INTERSECTION
PLE	PERMANENT LIMITED EASEMENT
PNT	POINT
PT	POINT OF TANGENT
R	RADIUS OF CURVE
R/L	REFERENCE LINE
R/W	RIGHT OF WAY
RC	REVERSE CROWN
REQ'D	REQUIRED
RHF	RIGHT HAND FORWARD
RO	RUN OFF LENGTH
RT	RIGHT
SALV	SALVAGED
SDD	STANDARD DETAIL DRAWINGS
SE	SUPER ELEVATION
SHLD	SHOULDER
STA	STATION
T	TANGENT LENGTH
TEL	TELEPHONE
TLE	TEMPORARY LIMITED EASEMENT
TYP	TYPICAL
VCL	VERTICAL CURVE LENGTH
VCP	POINT OF VERTICAL CURVE
VPI	POINT OF VERTICAL INTERSECTION
VPT	POINT OF VERTICAL TANGENT
WB	WESTBOUND

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

UTILITY CONTACTS

Vernon Electric Cooperative -
Electrical Distribution
Craig Buros
110 Saugstad Rd.
Westby, WI 54667
(608) 634-3121
cburos@vernonelectric.org

Vernon Communications Cooperative -
Communications
Scott Frederick
PO Box 20
Westby, WI 54667
(608) 634-7434
sfrederick@vernoncom.coop

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

ASPHALTIC SURFACE WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

4-INCH ASPHALTIC SURFACE SHALL BE CONSTRUCTED WITH 1.75-INCH UPPER LAYER AND 2.25-INCH LOWER LAYER

PRIOR TO ORDERING DRAINAGE PIPES AND STRUCTURES, THE CONTRACTOR SHALL VERIFY RELATED DRAINAGE INFORMATION IN THE PLAN AND PROVIDE DOCUMENTATION TO THE ENGINEER IN ACCORDANCE WITH THE SPECIFICATIONS. THIS ALSO INCLUDES VERIFICATIONS OF INVERT ELEVATIONS AT ALL PROPOSED CONNECTION POINTS TO EXISTING SYSTEMS.

THE CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY THE OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

PLACE TOPSOIL IN ALL GRADED AREAS AS DESIGNATED BY THE ENGINEER. SEED, FERTILIZE, AND PLACE EROSION MAT ALL AREAS WITHIN 5 DAYS OF PLACEMENT OF TOPSOIL.

TEMPORARY STORAGE OF ANY EXCAVATED MATERIAL OR EQUIPMENT WILL NOT BE PERMITTED IN WETLANDS, FLOODWAY, OR FLOODPLAIN OF ANY WATERWAY.

THE EROSION CONTROL FEATURES AS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER.

EROSION CONTROL DEVICES SHALL BE PLACED IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

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RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER	0 - 2	2 - 6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	0.70 - 0.95											
CONCRETE	0.80 - 0.95											
BRICK	0.70 - 0.80											
DRIVES, WALKS	0.75 - 0.85											
ROOFS	0.75 - 0.95											
GRAVEL ROADS, SHOULDERS	0.40 - 0.60											

TOTAL PROJECT AREA = 0.20 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.20 ACRES

WISDOT

WISDOT SOUTHWEST REGION (LA CROSSE)
ALEIGHA BURG
3550 MORMON COULEE ROAD
LA CROSS, WI 54601
(608) 317-9083
ALEIGHA.BURG@DOT.WI.GOV

CONSULTANT

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CHAD HALVERSON, P.E.
5400 KING JAMES WAY, SUITE 200
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(608) 663-1218
CHALVERSON@KLENGINEERING.COM

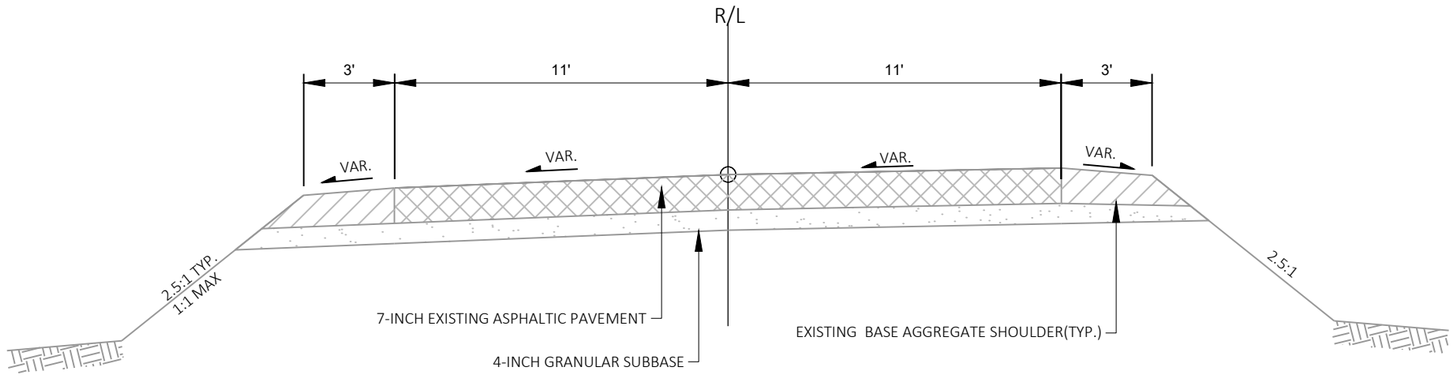
HIGHWAY COMMISSIONER

VERNON COUNTY
PHIL HEWITT
PO BOX 232
VIROQUA, WI 54665
(608) 637-5452
PHIL.HEWITT@VERNONCOUNTY.ORG

WISC. DEPT OF NATURAL RESOURCES

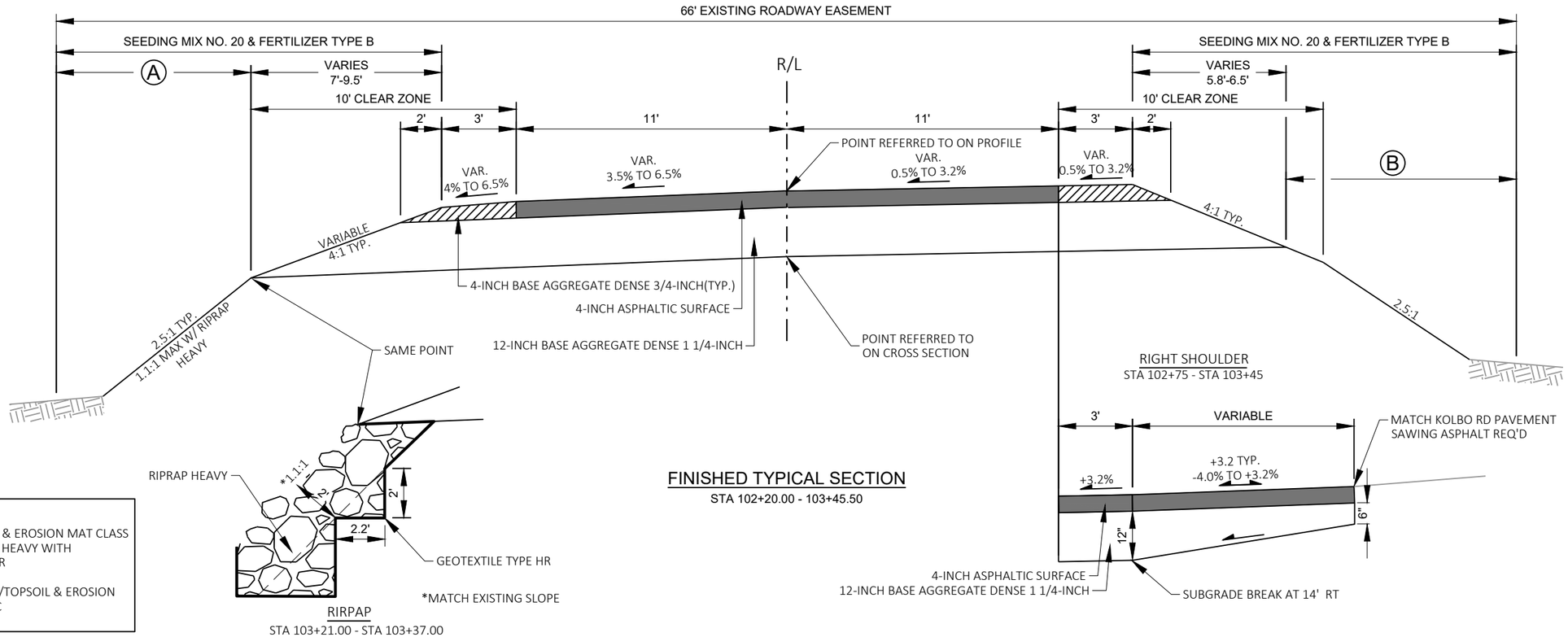
CENTRAL REGION
KAREN KALVELAGE
3550 MORMON COULEE ROAD
LA CROSSE, WI 54601
(608) 406-7880
karen.kalvelage@wisconsin.gov

FILE NAME : \\KLENGINEERING\DATA1\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\020101-GN.DWG
LAYOUT NAME - GEN NOTES
PLOT DATE : 11/2/2020 7:59 AM PLOT BY : KL ENGINEERING PLOT NAME : PLOT SCALE : 1 IN:100 FT
WISDOT/CADDS SHEET 42



EXISTING TYPICAL SECTION

STA 102+20.00 - 103+45.50



FINISHED TYPICAL SECTION

STA 102+20.00 - 103+45.50

RIRPAP

STA 103+21.00 - STA 103+37.00

(A) SALVAGED TOPSOIL & EROSION MAT CLASS II TYPE C OR RIPRAP HEAVY WITH GEOTEXTILE TYPE HR

(B) SALVAGED TOPSOIL/TOPSOIL & EROSION MAT CLASS II TYPE C

PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	TYPICAL SECTIONS	SHEET 3	E

N

*SIGN LOCATED NEAR STH 27/CTH P INTERSECTION

MATCH EXISTING

1R-01

1R-02

1R-03

ESTR

1R-07

COUNTY P
40 TON BRIDGE
16 MILES AHEAD

WEIGHT LIMIT 40 TONS

MATCH EXISTING

MATCH EXISTING

CTH P

1

103+00'P'

MATCH EXISTING

MATCH EXISTING

102+67.37'P', 11.00' RT

2

(DOUBLE)

3

MATCH EXISTING

1R-04

1R-05

1R-06

GUY

ANZ

WZ

TIMBER COULEE CREEK

KOLBO RD

ESTR

STOP

LEGEND

1	MARKING LINE EPOXY 4-INCH (WHITE)
2	MARKING LINE EPOXY 4-INCH (WHITE - 3' LINE, 9' GAP)
3	MARKING LINE EPOXY 4-INCH (YELLOW)

NOTE: PROPOSED LINEWORK SHOWN AS SCREENED FOR CLARITY

EXISTING SIGN MOUNTED ON POST(S)

XR-XX DENOTES SIGN TO BE REMOVED

ESTR EXISTING SIGN TO REMAIN

PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	PERMANENT SIGNING & PAVEMENT MARKING	SHEET 4	E

FILE NAME : G:\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\024501-PM.DWG
LAYOUT NAME - Plan 1 IN 10 FT

PLOT DATE : 5/26/2020 2:12 PM PLOT BY : KL ENGINEERING PLOT NAME : PLOT SCALE : 1 IN:10 FT

2

GENERAL NOTES FOR TRAFFIC CONTROL

1) THE EXACT NUMBER, LOCATION AND SPACING OF SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

2) ALL SIGNS ARE 48" X 48" UNLESS NOTED.

3) "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

4) FOR NIGHTTIME OPERATION, ALL DRUMS IN TAPERS SHALL HAVE A TYPE C STEADY BURN WARNING LIGHT.

5) ALL TYPE III BARRICADES SHALL BE 8' WIDE, UNLESS OTHERWISE NOTED, AND EQUIPPED WITH TWO TYPE "A" (LOW INTENSITY FLASHING) LIGHTS.

6) MAINTAIN LOCAL ACCESS TO KOLBO ROAD AT ALL TIMES UNLESS OTHERWISE NOTED IN THE TRAFFIC CONTROL PLANS.

TEMPORARY PAVEMENT MARKING LEGEND

T18W	TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18-INCH (WHITE)

TRAFFIC CONTROL LEGEND

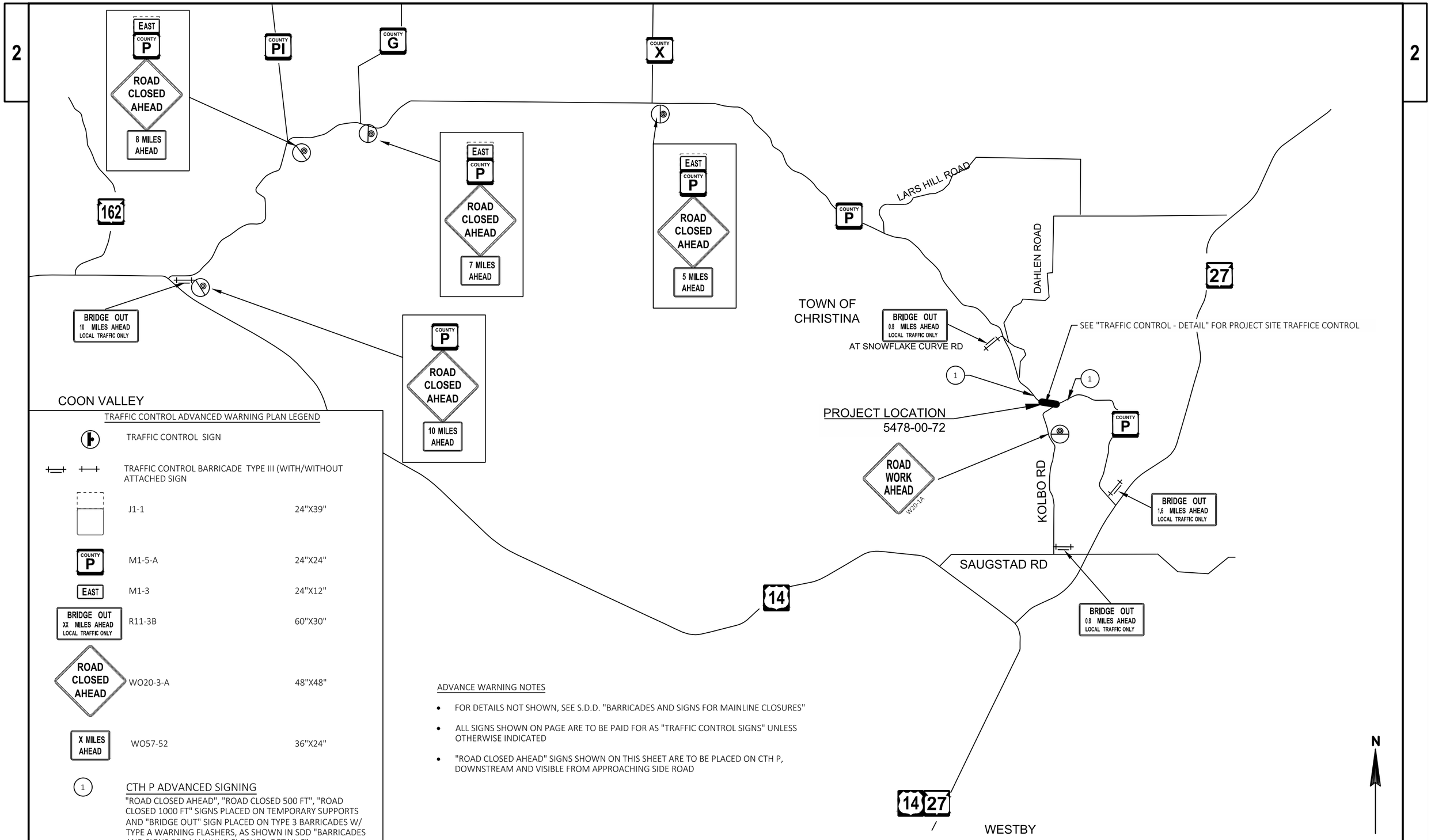
- TRAFFIC CONTROL BARRICADE TYPE III
- TRAFFIC CONTROL BARRICADE TYPE III (WITH ATTACHED SIGN)
- TRAFFIC CONTROL SIGNS (ON PERMANENT SUPPORT)
- TRAFFIC CONTROL SIGNS (ON TEMPORARY SUPPORT)
- TRAFFIC CONTROL DRUMS (WITH WARNING LIGHTS TYPE C)
- WORK AREA
- FLAGS, 16"X16" MIN. (ORANGE)

PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	TRAFFIC CONTROL - NOTES & LEGEND	SHEET 5	E

FILE NAME : \\KLENGINEERING\DATA1\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\TRAFFIC CONTROL\NOTES & LEGEND.DWG
LAYOUT NAME - Plan 1 IN 100 FT

PLOT DATE : 7/1/2020 9:43 AM PLOT BY : KL ENGINEERING PLOT NAME : PLOT SCALE : 1:1

WISDOT/CADDS SHEET 42



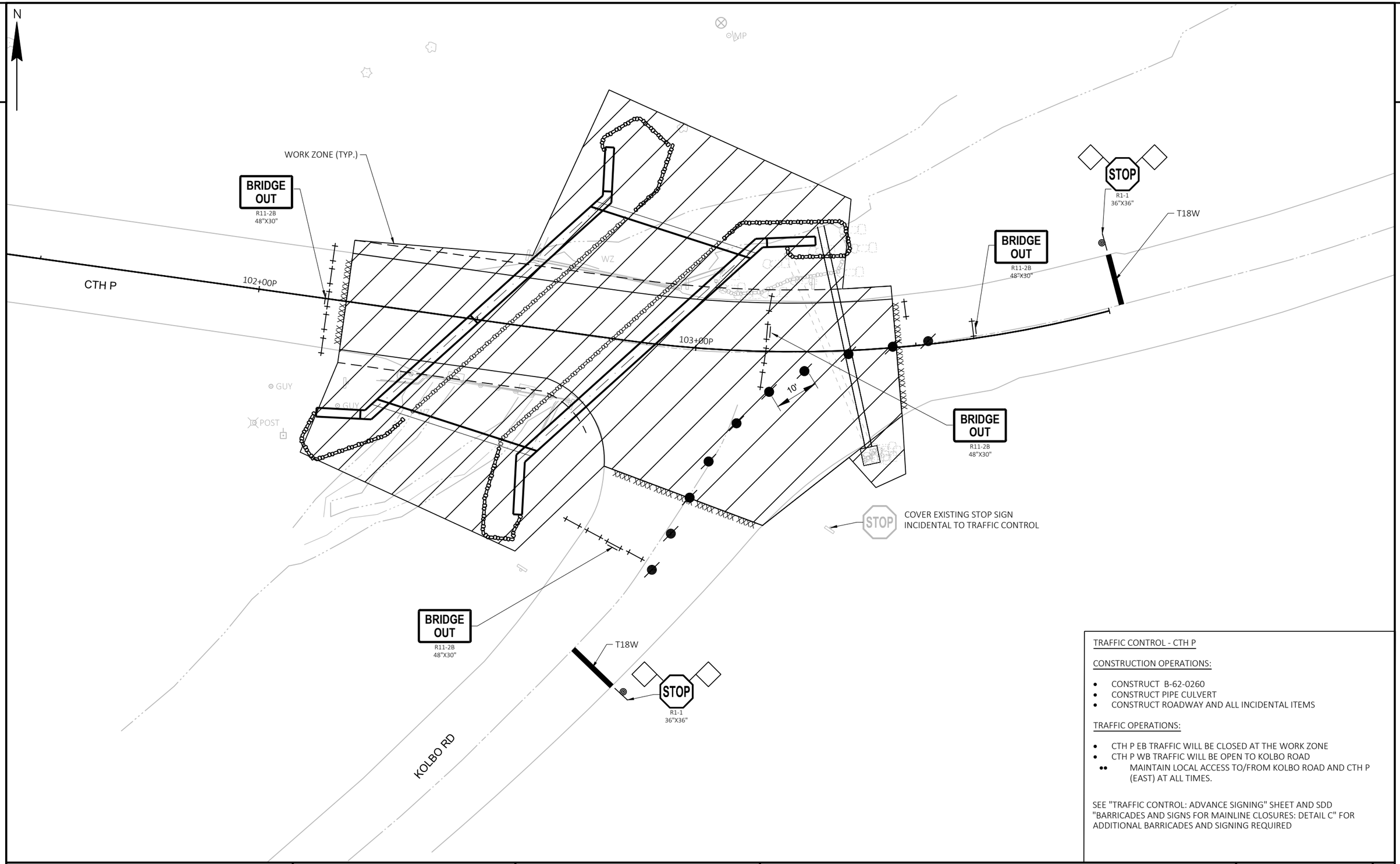
TRAFFIC CONTROL ADVANCED WARNING PLAN LEGEND

	TRAFFIC CONTROL SIGN	
	TRAFFIC CONTROL BARRICADE TYPE III (WITH/WITHOUT ATTACHED SIGN	
	J1-1	24"X39"
COUNTY P	M1-5-A	24"X24"
EAST	M1-3	24"X12"
BRIDGE OUT XX MILES AHEAD LOCAL TRAFFIC ONLY	R11-3B	60"X30"
ROAD CLOSED AHEAD	WO20-3-A	48"X48"
X MILES AHEAD	WO57-52	36"X24"
1	CTH P ADVANCED SIGNING "ROAD CLOSED AHEAD", "ROAD CLOSED 500 FT", "ROAD CLOSED 1000 FT" SIGNS PLACED ON TEMPORARY SUPPORTS AND "BRIDGE OUT" SIGN PLACED ON TYPE 3 BARRICADES W/ TYPE A WARNING FLASHERS, AS SHOWN IN SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURE, DETAIL C"	

ADVANCE WARNING NOTES

- FOR DETAILS NOT SHOWN, SEE S.D.D. "BARRICADES AND SIGNS FOR MAINLINE CLOSURES"
- ALL SIGNS SHOWN ON PAGE ARE TO BE PAID FOR AS "TRAFFIC CONTROL SIGNS" UNLESS OTHERWISE INDICATED
- "ROAD CLOSED AHEAD" SIGNS SHOWN ON THIS SHEET ARE TO BE PLACED ON CTH P, DOWNSTREAM AND VISIBLE FROM APPROACHING SIDE ROAD

PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	TRAFFIC CONTROL - ADVANCED SIGNING	SHEET 6	E



TRAFFIC CONTROL - CTH P

CONSTRUCTION OPERATIONS:

- CONSTRUCT B-62-0260
- CONSTRUCT PIPE CULVERT
- CONSTRUCT ROADWAY AND ALL INCIDENTAL ITEMS

TRAFFIC OPERATIONS:

- CTH P EB TRAFFIC WILL BE CLOSED AT THE WORK ZONE
- CTH P WB TRAFFIC WILL BE OPEN TO KOLBO ROAD
  - MAINTAIN LOCAL ACCESS TO/FROM KOLBO ROAD AND CTH P (EAST) AT ALL TIMES.

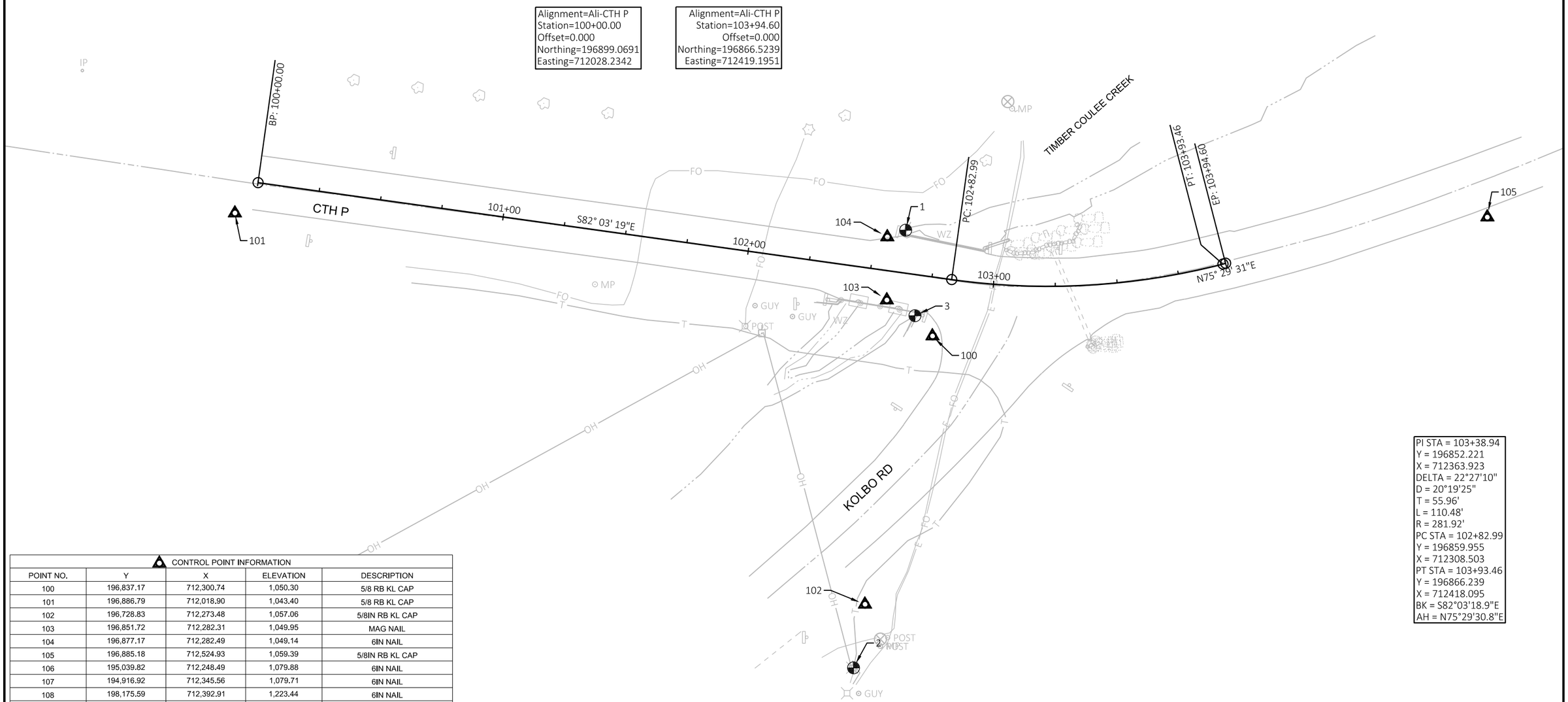
SEE "TRAFFIC CONTROL: ADVANCE SIGNING" SHEET AND SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES: DETAIL C" FOR ADDITIONAL BARRICADES AND SIGNING REQUIRED

PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	TRAFFIC CONTROL - DETAIL	SHEET 7	E

2

N

BENCHMARK INFORMATION				
POINT NO.	Y	X	ELEVATION	DESCRIPTION
1	196,879.86	712,289.92	1,049.83	USGS DISK NW WWAL CTH P BRIDGE
2	196,703.18	712,268.81	1,060.37	RR SPIKE PPOL #4F16R24
3	196,845.34	712,293.62	1,050.02	CUT SQ SE CORN CTH P BRIDGE



PI STA = 103+38.94
Y = 196852.221
X = 712363.923
DELTA = 22°27'10"
D = 20°19'25"
T = 55.96'
L = 110.48'
R = 281.92'
PC STA = 102+82.99
Y = 196859.955
X = 712308.503
PT STA = 103+93.46
Y = 196866.239
X = 712418.095
BK = S82°03'18.9"E
AH = N75°29'30.8"E

CONTROL POINT INFORMATION				
POINT NO.	Y	X	ELEVATION	DESCRIPTION
100	196,837.17	712,300.74	1,050.30	5/8 RB KL CAP
101	196,886.79	712,018.90	1,043.40	5/8 RB KL CAP
102	196,728.83	712,273.48	1,057.06	5/8IN RB KL CAP
103	196,851.72	712,282.31	1,049.95	MAG NAIL
104	196,877.17	712,282.49	1,049.14	6IN NAIL
105	196,885.18	712,524.93	1,059.39	5/8IN RB KL CAP
106	195,039.82	712,248.49	1,079.88	6IN NAIL
107	194,916.92	712,345.56	1,079.71	6IN NAIL
108	198,175.59	712,392.91	1,223.44	6IN NAIL
109	198,202.02	712,759.47	1,232.00	6IN NAIL
110	197,916.25	712,362.45	1,185.86	6IN NAIL

NOTE: POINTS 106, 107, 108, 109, & 110 ARE NOT SHOWN ON THE PLAN

PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	ALIGNMENT DIAGRAM & CONTROL POINT DIAGRAM	SHEET 8	E

5478-00-72

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	1.000	1.000
0004	203.0600.S	Removing Old Structure Over Waterway With Minimal Debris (station) 01. 102+75	LS	1.000	1.000
0006	205.0100	Excavation Common	CY	210.000	210.000
0008	206.1000	Excavation for Structures Bridges (structure) 01. B-62-260	LS	1.000	1.000
0010	206.5000	Cofferdams (structure) 01. B-62-260	LS	1.000	1.000
0012	208.0100	Borrow	CY	50.000	50.000
0014	210.1500	Backfill Structure Type A	TON	1,380.000	1,380.000
0016	213.0100	Finishing Roadway (project) 01. 5478-00-72	EACH	1.000	1.000
0018	305.0110	Base Aggregate Dense 3/4-Inch	TON	16.000	16.000
0020	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	360.000	360.000
0022	455.0605	Tack Coat	GAL	32.000	32.000
0024	465.0105	Asphaltic Surface	TON	105.000	105.000
0026	502.0100	Concrete Masonry Bridges	CY	298.000	298.000
0028	502.3200	Protective Surface Treatment	SY	76.000	76.000
0030	505.0400	Bar Steel Reinforcement HS Structures	LB	9,760.000	9,760.000
0032	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	42,750.000	42,750.000
0034	511.1200	Temporary Shoring (structure) 01. B-62-260	SF	184.000	184.000
0036	516.0500	Rubberized Membrane Waterproofing	SY	29.000	29.000
0038	516.0610.S	Sheet Membrane Waterproofing for Top Slab (structure) 01. B-62-260	SY	254.000	254.000
0040	521.3118	Culvert Pipe Corrugated Steel 18-Inch	LF	51.000	51.000
0042	550.0500	Pile Points	EACH	28.000	28.000
0044	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	700.000	700.000
0046	606.0300	Riprap Heavy	CY	141.000	141.000
0048	611.0642	Inlet Covers Type MS	EACH	1.000	1.000
0050	611.3901	Inlets Median 1 Grate	EACH	1.000	1.000
0052	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	225.000	225.000
0054	618.0100	Maintenance And Repair of Haul Roads (project) 01. 5478-00-72	EACH	1.000	1.000
0056	619.1000	Mobilization	EACH	1.000	1.000
0058	624.0100	Water	MGAL	14.000	14.000
0060	625.0100	Topsoil	SY	45.000	45.000
0062	625.0500	Salvaged Topsoil	SY	125.000	125.000
0064	628.1504	Silt Fence	LF	230.000	230.000
0066	628.1520	Silt Fence Maintenance	LF	700.000	700.000
0068	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0070	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0072	628.2027	Erosion Mat Class II Type C	SY	225.000	225.000
0074	628.7020	Inlet Protection Type D	EACH	1.000	1.000

5478-00-72

Line	Item	Item Description	Unit	Total	Qty
0076	629.0210	Fertilizer Type B	CWT	1.250	1.250
0078	630.0120	Seeding Mixture No. 20	LB	5.000	5.000
0080	630.0200	Seeding Temporary	LB	6.000	6.000
0082	630.0500	Seed Water	MGAL	5.000	5.000
0084	633.5200	Markers Culvert End	EACH	1.000	1.000
0086	638.2602	Removing Signs Type II	EACH	7.000	7.000
0088	638.3000	Removing Small Sign Supports	EACH	6.000	6.000
0090	642.5001	Field Office Type B	EACH	1.000	1.000
0092	643.0300	Traffic Control Drums	DAY	770.000	770.000
0094	643.0420	Traffic Control Barricades Type III	DAY	1,827.000	1,827.000
0096	643.0705	Traffic Control Warning Lights Type A	DAY	2,807.000	2,807.000
0098	643.0715	Traffic Control Warning Lights Type C	DAY	770.000	770.000
0100	643.0900	Traffic Control Signs	DAY	2,562.000	2,562.000
0102	643.5000	Traffic Control	EACH	1.000	1.000
0104	645.0111	Geotextile Type DF Schedule A	SY	170.000	170.000
0106	645.0120	Geotextile Type HR	SY	206.000	206.000
0108	646.1020	Marking Line Epoxy 4-Inch	LF	500.000	500.000
0110	649.0850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	24.000	24.000
0112	650.4000	Construction Staking Storm Sewer	EACH	1.000	1.000
0114	650.4500	Construction Staking Subgrade	LF	126.000	126.000
0116	650.5000	Construction Staking Base	LF	126.000	126.000
0118	650.6000	Construction Staking Pipe Culverts	EACH	1.000	1.000
0120	650.6500	Construction Staking Structure Layout (structure) 01. B-62-260	LS	1.000	1.000
0122	650.9910	Construction Staking Supplemental Control (project) 01. 5478-00-72	LS	1.000	1.000
0124	650.9920	Construction Staking Slope Stakes	LF	186.000	186.000
0126	690.0150	Sawing Asphalt	LF	87.000	87.000
0128	715.0502	Incentive Strength Concrete Structures	DOL	1,818.000	1,818.000
0130	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	1,200.000	1,200.000
0132	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	600.000	600.000

3

REMOVING SMALL PIPE CULVERTS

203.0100

CATEGORY	LOCATION	STATION	OFFSET	(EACH)	SIZE / TYPE
0010	CTH P				
		103+30	---	1	18-INCH CMCP
	PROJECT 5478-00-72 TOTAL			1	

EARTHWORK

		205.0100 Excavation Common (1)		Salvaged/ Unusable		Reduced	Unexpanded	Expanded	Mass	Waste	208.0100
			EBS	P	A	EBS	....	Fill	O		B
CATEGORY	LOCATION	C 2	E 3	M 4	M 5	8 Factor 0.80		10 Factor 1.25	11		
		CY	CY	CY	CY	CY	CY	CY	CY	CY	CY
0010	CTH P	200	10	---	200	8	208	24	-50	0	
		200	10	---	200	8	208	24	-50	0	50
T E C		210									

1 C E C EBS E

2 SU P M C

3 EBS E S C M

4 SU P M

5 A M C - SU P M

6 R EBS - E EBS 11 . EBS R 0.8

7 E . 1.25

8 T M O - D. P D. M
D.

BASE AGGREGATE DENSE

CATEGORY	LOCATION	STATION TO STATION	305.0110 3/4 - INCH (TON)	305.0120 1 1/4- INCH (TON)
0010	CTH P			
		102+20.00 - 103+45.50	16	360
	PROJECT 5478-00-72 TOTAL		16	360

ASPHALTIC ITEMS

CATEGORY	LOCATION	STATION TO STATION	455.0605 TACK COAT GAL	465.0105 ASPHALTIC SURFACE (TON)
0010	CTH P			
		102+20.00 - 103+45.50	32	105
	PROJECT 5478-00-72 TOTAL		32	105

3

PROJECT NO 5478-00-72	HWY CTH P	COUNTY VERNON	MISCELLANEOUS UANTITIES	SHEET 11	E

CULVERT PIPE

						521.3118	633.5200	650.6000		
						CULVERT PIPE CORRUGATED STEEL	MARKERS CULVERT	CONSTRUCTION STAKING		
CATEGORY	LOCATION	STATION	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE TT	18-INCH L	END EACH	PIPE CULVERTS EACH	THICKNESS INCHES STEEL	ALUMINUM
0010	CTH P									
		10334	104.08	1041.3	0.1403	51	1	1	0.064	0.060
	PROJECT 5478-00-72 TOTAL					51	1	1	0	0

CULVERT STRUCTURE

							611.0642	611.301	650.4000	
									CONSTRUCTION	
							INLET COVERS	INLETS MEDIAN	STAKING	
				RIM	INVERT	DEPTH	TYPE MS	1 GRATE	STORM SEWER	
CATEGORY	LOCATION	STATION	OSET	ELEVATION	ELEVATION	T	EACH	EACH	EACH	COMMENTS
0010	CTH P									
		10338	22.0 RT	1052.08	104.08	3.00	1	1	1	61 SINGLE SLOPE
	PROJECT 5478-00-72 TOTAL						1	1	1	

OR STRUCTURES WITH SUMPS THE INVERT ELEVATION IS THE ELEVATION O THE SUMP. OR STRUCTURES WITHOUT SUMPS THE INVERT ELEVATION IS THE ELEVATION O THE LOWEST PIPE LOW LINE

3

LANDSCAPING

CATEGORY	LOCATION	STATION TO STATION	OFFSET	625.0100 TOPSOIL (SY)	625.0500 SALVAGED TOPSOIL (SY)	628.2027 EROSION MAT CLASS II TYPE C (SY)	629.0210 FERTILIZER TYPE B (CWT)	630.0120 SEEDING MIXTURE NO. 20 (LB)	630.0200 SEEDING TEMPORARY (LB)	630.0500 SEED WATER (MGAL)
0010	CTH P									
		102+20.00 - 103+45.50	LT	19	61	102	0.58	1.84	0	2
		102+20.00 - 103+45.50	RT	22	58	80	0.45	1.44	0	2
		UNDISTRIBUTED		4	7	43	0.22	1.72	6.00	1
	PROJECT 5478-00-72 TOTAL			45	125	225	1.25	5	6	5

WATER

CATEGORY	LOCATION	TASK	624.0100 (MGAL)
0010	CTH P	DUST CONTROL	7
		COMPACTION	7
	PROJECT 5478-00-72 TOTAL		14

EROSION CONTROL

CATEGORY	LOCATION	STATION TO STATION	OFFSET	628.1504 SILT FENCE (LF)	628.1520 SILT FENCE MAINTENANCE (LF)	628.7020 INLET PROTECTION TYPE D (EACH)
0010	CTH P					
		102+20 - 103+45.50	LT	130	390	---
		102+20 - 103+45.50	RT	55	165	1
		UNDISTRIBUTED		45	145	---
	PROJECT 5478-00-72 TOTAL			230	700	1

EROSION CONTROL MOBILZATION

CATEGORY	LOCATION	628.1905 MOBILIZATION EROSION CONTROL (EACH)	628.1910 MOBILIZATION EMERGENCY EROSION CONTROL (EACH)
0010	CTH P	3	2
	PROJECT 5478-00-72 TOTAL	3	2

PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	MISCELLANEOUS QUANTITIES	SEET 13	E

3

REMOVING SIGNS

CATEGORY	SIGN #	SIGN CODE	638.2602 REMOVING SIGNS TYPE II (EACH)	638.3000 REMOVING SMALL SIGN SUPPORTS (EACH)	SIGN MOUNTED ON SAME POST AS	COMMENTS
0010	1R-01	W5-52-L	.	1	---	AAR PANEL LEFT
	R-02	5-52-R	1	1	---	AAR PANEL RIGT
	R-0	R2-	1	1	---	EIGT LIMIT 40 TONS
	R-04	R2-	1	1	---	EIGT LIMIT 40 TONS
	R-05	5-52-R	1	1	---	AAR PANEL RIGT
	R-06	5-52-L	1	1	---	AAR PANEL LEFT
	R-07	R2-55	1	---	CT P SIGN	40 TON RIGE 6 MILES AEA
	PROJECT 5478-00-72 TOTAL		7	6		

PAVEMENT MARKING

646.1020

MARKING LINE

EPOXY 4-INCH

CATEGORY	LOCATION	WHITE SOLID (LF)	WHITE (3' LINE 9' GAP) (LF)	YELLOW SOLID (LF)
0010	CTH P	170	80	250
	PROJECT 5478-00-72 TOTAL	170	80	250
			500	

3

TRAFFIC CONTROL

CATEGOR	STAGE	NO OF AS A	RMS TPE C LIGT EAC	64000 RMS A	64075 ARNING LIGTS TPE C A	ARRICAES TPE III EAC	640420 ARRICAES TPE III A	ARNING LIGTS TPE A EAC	640705 ARNING LIGTS TPE A A	SIGNS EAC	64000 SIGNS A
000	AVANCE ARNING	84	--	--	--	8	672	6	44	25	200
	STAGE	77	0	770	770	5	55	19	46	6	462
	PROJECT TOTAL	84		770	770		827		2807		2562

TEMPORARY PAVEMENT MARKING

CATEGORY	LOCATION	649.0850 TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18-INCH (LF)
0010	CTH P	
	CTH P	12
	KOLBO ROAD	12
	PROJECT 5478-00-72 TOTAL	24

CONSTRUCTION STAKING

CATEGORY	LOCATION	STATION - STATION	650.4500 CONSTRUCTION STAKING SUBGRADE (LF)	650.5000 CONSTRUCTION STAKING BASE (LF)	650.6500 CONSTRUCTION STAKING STRUCTURE LAYOUT (B-62-0260) (LS)	650.9920 CONSTRUCTION STAKING SLOPE STAKES (LF)
0010	CTH P					
		102+20.00 - 103+45.50	126	126	1	186
	PROJECT 5478-00-72 TOTAL		126	126	1	186

SAWING ASPHALT

CATEGORY	LOCATION	STATION	690.0150 (LF)
0010	CTH P		
		102+20.00	22
		KOBLO ROAD	38
		103+45.50	27
	PROJECT 5478-00-72 TOTAL		87

CONVENTIONAL SYMBOLS

SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W LINE
PROPERTY LINE
LOT, TIE, AND OTHER MINOR LINES
SLOPE INTERCEPT
CORPORATE LIMITS
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)
FEE ACQUISITION AREA (HATCHING VARIES BY OWNER)
TEMP. LIMITED EASEMENT AREA
EASEMENT AREA (HIGHWAY, PERMANENT LIMITED, OR RESTRICTED DEVELOPMENT)
TRANSMISSION STRUCTURES
BUILDING
BUILDING (TO BE REMOVED)
BRIDGE

PARCEL NUMBER 25
SECTION CORNER
NOTATION FOR COMBUSTABLE FLUIDS
NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES
UTILITY NUMBER 40
R/W MONUMENT
NON-MONUMENTED R/W POINT
FOUND IRON PIN (1" UNLESS NOTED) IP
VALVE (GAS, WATER, ETC.)
SIGN
OFF-PREMISE SIGN

ACCESS CONTROLLED BY ACQUISITION
NO ACCESS (BY STATUTORY AUTHORITY)
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)
NO ACCESS (NEW HIGHWAY)
NATIONAL GEODETIC SURVEY MONUMENT
SIXTEENTH CORNER MONUMENT
PARALLEL OFFSETS

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	OUTLOT	OL
ACRES	AC	PAGE	P
AHEAD	AH	POINT OF TANGENCY	PT
ALUMINUM	ALUM	PROPERTY LINE	PL
AND OTHERS	ET AL	RECORDED AS	(100')
BACK	BK	REEL / IMAGE	R/I
BLOCK	BLK	REFERENCE LINE	R/L
CENTERLINE	C/L	PERMANENT LIMITED EASEMENT	PLE
CERTIFIED SURVEY MAP	CSM	POINT OF BEGINNING	POB
CONCRETE	CONC	POINT OF CURVATURE	PC
COUNTY	CO	POINT OF COMPOUND CURVE	PCC
COUNTY TRUNK HIGHWAY	CTH	POINT OF INTERSECTION	PI
DISTANCE	DIST	REMAINING	REM
CORNER	COR	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
DOCUMENT NUMBER	DOC	RIGHT	RT
EASEMENT	EASE	RIGHT OF WAY	R/W
EXISTING	EX	SECTION	SEC
GAS VALVE	GV	SEPTIC VENT	SEPV
GRID NORTH	GN	SQUARE FEET	SF
HIGHWAY EASEMENT	HE	STATE TRUNK HIGHWAY	STH
IDENTIFICATION	ID	STATION	STA
LAND CONTRACT	LC	TELEPHONE PEDESTAL	TP
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT PLAT	TPP
NATIONAL GEODETIC SURVEY NUMBER	NGS	UNITED STATES HIGHWAY	USH
	NO	VOLUME	V

CONVENTIONAL UTILITY SYMBOLS

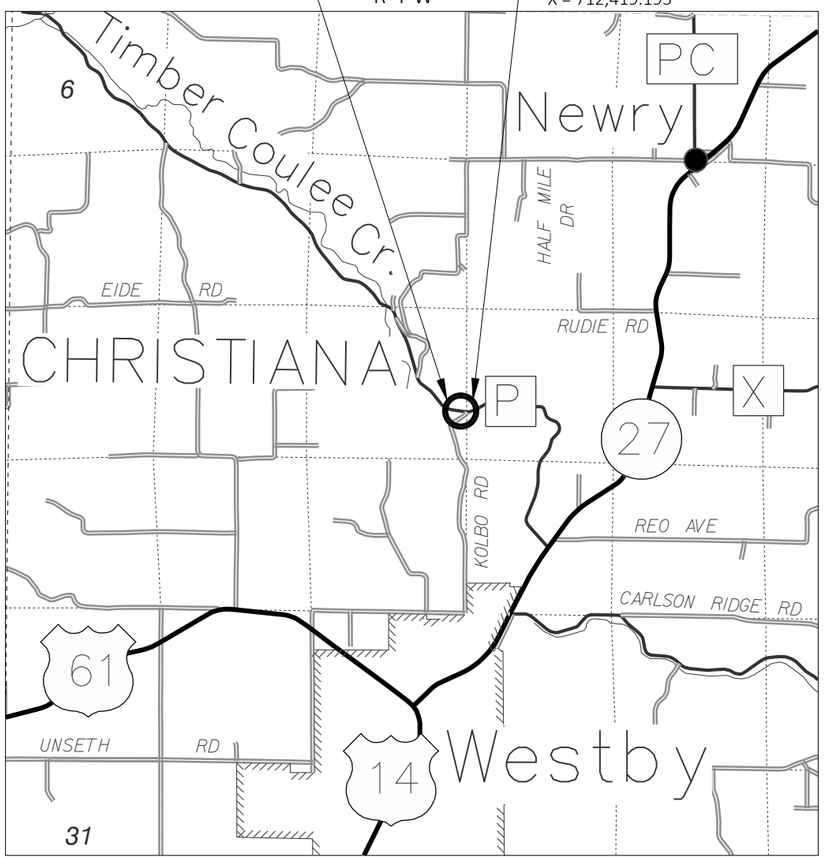
WATER	W	
GAS	G	
TELEPHONE	T	
OVERHEAD TRANSMISSION LINES	OH	
ELECTRIC	E	
CABLE TELEVISION	TV	
FIBER OPTIC	FO	
SANITARY SEWER	SAN	
STORM SEWER	SS	
ELECTRIC TOWER		
	NON-COMPENSABLE	COMPENSABLE
POWER POLE		
TELEPHONE POLE		
TELEPHONE PEDESTAL		

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

BEGIN RELOCATION ORDER
STA. 102+00.00
889.57' SOUTH AND 93.91' WEST OF THE EAST ¼ CORNER OF SECTION 16, T 14 N, R 4 W
Y = 196,871.425
X = 712,226.315

END RELOCATION ORDER
STA. 103+94.60
894.47' SOUTH AND 98.71' EAST OF THE EAST ¼ CORNER OF SECTION 16, T 14 N, R 4 W
Y = 196,866.524
X = 712,419.195



LAYOUT
SCALE 0 1MI

TOTAL NET LENGTH OF REFERENCE LINE = 0.037 MI

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
5478-00-00		
FEDERAL PROJECT NUMBER	4.01	3

PLAT OF RIGHT OF WAY REQUIRED FOR

TIMBER COULEE CREEK BRIDGE, B-62-0260
(CTH X - STH 27)

CTH P — VERNON COUNTY

CONSTRUCTION PROJECT NUMBER
5478-00-72

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), VERNON COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 MONUMENTS (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD."

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO NEW REFERENCE LINES.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE HIGHWAY DEPARTMENT OF VERNON COUNTY.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON DETAIL SHEETS.

THIS PLAT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES AND ACCESS RIGHTS.

REVISION DATE

KL Engineering
[A] Better Experience

I HEREBY CERTIFY THAT THIS PLAT WAS CREATED FOR VERNON COUNTY, WISCONSIN AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

WISCONSIN TIMOTHY M. HELD S-2591 MADISON WI LAND SURVEYOR

DATE SIGNED: 03/30/2020

APPROVED FOR VERNON COUNTY
DATE: 4-6-20 Phil Hewitt Signature

UNOFFICIAL DOCUMENT

FILE NAME : G:\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\040101-TITLE.DWG
PLOT DATE : 3/30/2020 10:38 AM
PLOT BY : TIMOTHY HELD
PLOT NAME :

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO VERNON COUNTY.

PARCEL NUMBER	SHEET NUMBER	OWNER(S)	INTEREST REQUIRED	R/W REQUIRED (S.F.)			
				NEW	EXISTING	TOTAL	TLE S.F.
1	4.03	DANIEL H. SEVERSON AND SUSAN R. SEVERSON	FEE	126	3299	3425	----
2	4.03	STATE OF WISCONSIN DEPARTMENT OF NATURAL RESOURCES	TLE	----	----	----	180
3	4.03	JAMES R. ONSHUS	FEE/TLE	228	2322	2550	347
4	4.03	VERNON COUNTY	FEE	322	----	322	----
5	4.03	DAVID EGGEN AND MARY EGGEN	FEE	----	6738	6738	----
200	4.03	VERNON COMMUNICATIONS COOPERATIVE	RELEASE OF RIGHTS	----	----	----	----

NOTE: AREAS SHOWN IN THE TOTAL ACRES COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM THE TAX ROLES OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE AQUIRED.

4

4

UNOFFICIAL DOCUMENT

REVISION DATE _____ _____ _____ _____ _____ _____ _____ _____ _____ _____ _____ _____

DATE: 03/30/2020

GRID FACTOR _____

NOT TO SCALE

HWY: CTH P

COUNTY: VERNON

R/W PROJECT NUMBER 5478-00-00

CONSTRUCTION PROJECT NUMBER 5478-00-72

PLAT SHEET 4.02

PS&E SHEET 17

E

FILE NAME : 040101-RP.DWG
LAYOUT NAME - SCHEDULE SHEET (4.02)

PLOT DATE : 3/30/2020 10:43 AM PLOT BY : TIMOTHY HELD PLOT NAME : PLOT SCALE : 1 IN:50 FT

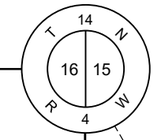
WISDOT/CADDS SHEET 75

TLE COURSE TABLE			
FROM POINT	TO POINT	BEARING	DISTANCE
300	301	S08° 14' 42"W	7.63'
301	302	S81° 45' 16"E	20.06'
302	109	S49° 15' 59"E	28.80'
109	110	N08° 14' 43"E	23.10'
110	300	N81° 45' 17"W	44.35'

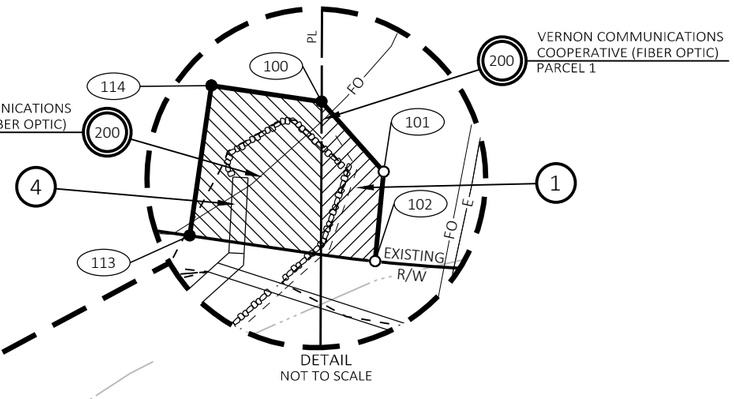
TLE Station & Offset Table		
Point No.	Station	Offset
300	102+10.04	32.37'
301	102+10.00	40.00'
302	102+30.06	40.10'

PI STA = 103+38.94
Y = 196852.221
X = 712363.923
DELTA = 22°27'10" LT
D = 20°19'25"
T = 55.96'
L = 110.48'
R = 281.92'
PC STA = 102+82.99
Y = 196859.955
X = 712308.503
PT STA = 103+93.46
Y = 196866.239
X = 712418.095

VERNON COUNTY SURVEY
MONUNMENT FOUND
Y = 197,760.993
X = 712,320.224



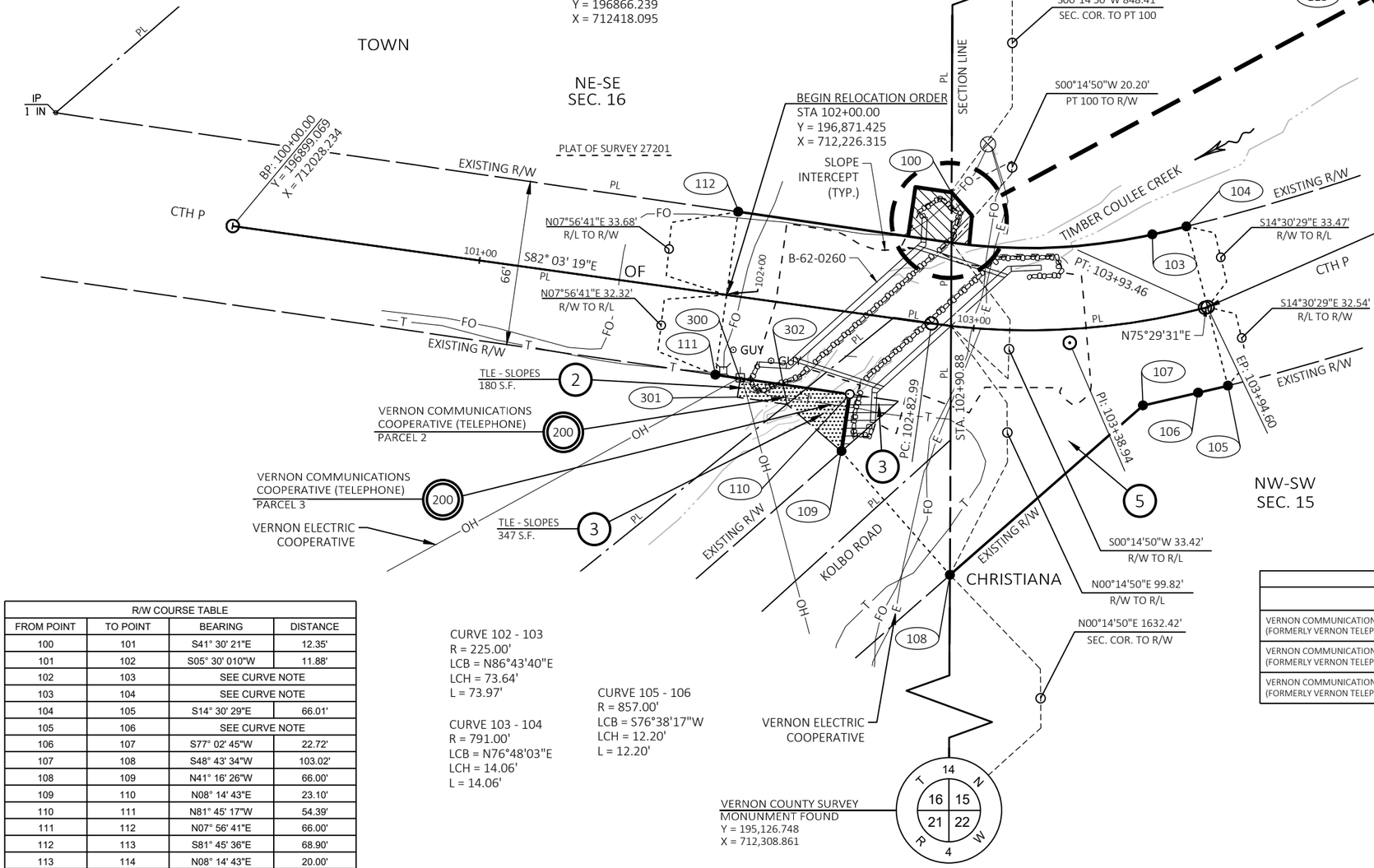
GN



TOWN

NE-SE
SEC. 16

PLAT OF SURVEY 27201



NW-SW
SEC. 15

END RELOCATION ORDER
STA 103+94.60
Y = 196,866.524
X = 712,419.195

R/W Station, Offset Table & Coordinate				
Point No.	Station	Offset	Y	X
100	102+83.86	53.24'	196912.592	712316.564
101	102+95.00	45.00'	196903.341	712324.750
102	102+95.00	33.12'	196891.514	712323.610
103	103+78.80	33.46'	196895.717	712397.125
104	103+94.60	33.47'	196898.927	712410.810
105	103+94.60	32.53'	196835.026	712427.346
106	103+83.54	32.48'	196832.206	712415.473
107	103+63.21	33.49'	196827.112	712393.328
108	102+98.71	99.40'	196759.154	712315.902
109	102+54.27	55.70'	196808.757	712272.365
110	102+54.39	32.60'	196831.618	712275.678
111	102+00.00	32.32'	196839.418	712221.848
112	102+00.00	33.68'	196904.786	712230.970
113	102+68.89	33.33'	196894.912	712299.154
114	102+69.00	53.33'	196914.705	712302.022

R/W COURSE TABLE			
FROM POINT	TO POINT	BEARING	DISTANCE
100	101	S41° 30' 21"E	12.35'
101	102	S05° 30' 010"W	11.88'
102	103	SEE CURVE NOTE	
103	104	SEE CURVE NOTE	
104	105	S14° 30' 29"E	66.01'
105	106	SEE CURVE NOTE	
106	107	S77° 02' 45"W	22.72'
107	108	S48° 43' 34"W	103.02'
108	109	N41° 16' 26"W	66.00'
109	110	N08° 14' 43"E	23.10'
110	111	N81° 45' 17"W	54.39'
111	112	N07° 56' 41"E	66.00'
112	113	S81° 45' 36"E	68.90'
113	114	N08° 14' 43"E	20.00'
114	100	S81° 43' 48"E	14.69'

CURVE 102 - 103
R = 225.00'
LCB = N86°43'40"E
LCH = 73.64'
L = 73.97'

CURVE 103 - 104
R = 791.00'
LCB = N76°48'03"E
LCH = 14.06'
L = 14.06'

CURVE 105 - 106
R = 857.00'
LCB = S76°38'17"W
LCH = 12.20'
L = 12.20'

VERNON COUNTY SURVEY
MONUNMENT FOUND
Y = 195,126.748
X = 712,308.861

EASEMENT TABLE		
OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.
VERNON COMMUNICATIONS COOPERATIVE (FORMERLY VERNON TELEPHONE COOPERATIVE)	BLANKET EASEMENT DOC. NO. 253731	1, 5
VERNON COMMUNICATIONS COOPERATIVE (FORMERLY VERNON TELEPHONE COOPERATIVE)	BLANKET EASEMENT DOC. NO. 253737	2, 3
VERNON COMMUNICATIONS COOPERATIVE (FORMERLY VERNON TELEPHONE COOPERATIVE)	NO EASEMENT OF RECORD	4

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), VERNON COUNTY, NAD 83(2011) IN U.S.SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR CURRENT ACCESS DRIVEWAY INFORMATION, CONTACT THE HIGHWAY DEPARTMENT OF VERNON COUNTY.

CTH P EXISTING R/W LINE WAS ESTABLISHED BY PLAT OF SURVEY 27201 BY CURTIS CROOK S-1292 AND SIGNED ON 04/04/1979, STATE STATUE 82.31 (66' WIDE).

KOLBO RD. EXISTING R/W LINE WAS ESTABLISHED BY STATE STATUE 82.18 (66' WIDE).

UNOFFICIAL DOCUMENT

REVISION DATE

DATE: 03/30/2020

GRID FACTOR

SCALE, FEET 0 25 50

HWY: CTH P

COUNTY: VERNON

R/W PROJECT NUMBER 5478-00-00

CONSTRUCTION PROJECT NUMBER 5478-00-72

PLAT SHEET 4.03

PS&E SHEET 18

E

FILE NAME : 040101-RP.DWG
LAYOUT NAME - DETAIL SHEET (4.03)

PLOT DATE : 3/30/2020 10:47 AM PLOT BY : TIMOTHY HELD PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 75

STATION	DESCRIPTION	LEFT LANE	RIGHT LANE
102+20.00	MATCH EXIST	-3.50%	0.50%
102+29.25	BEGIN SHOULDER ROLLOVER	-3.50%	0.50%
102+69.95	BEGIN FULL SUPER RIGHT	-4.00%	1.00%
102+75.50	BEGIN FULL SUPER LEFT	-6.20%	3.20%
103+28.35	END FULL SUPER RIGHT	-6.50%	3.20%
103+45.00	MATCH EXIST	-6.50%	2.30%

VERNON COUNTY

DANIEL H SEVERSON

DAVID EGGEN

JAMES ONSHUS

CTH P

N

102+00

103+00

REMOVING OLD STRUCTURE MINIMAL DEBRIS
STA 102+50 TO STA 102+85
SINGLE SPAN STEEL GIRDER
28'6" CLEAR ROADWAY WIDTH
31' OVERALL LENGTH

EXISTING RIGHT OF WAY

SLOPE INTERCEPT (TYP.)

SILT FENCE(TYP.)

EDGE OF 3' BASE AGGREGATE SHOULDER (TYP.)

EDGE OF ASPHALT (TYP.)

22'

BEGIN CONSTRUCTION
STA 102+20.00
MATCH EXISTING
SAW CUT REQ'D

TOPSOIL OVER STRUCTURE TO SHOULDER

EXISTING HIGHWAY EASEMENT

WETLAND BOUNDARY

VERNON COMMUNICATIONS COOP

VERNON ELECTRIC COOP

NEW STRUCTURE B-62-0260
SINGLE SPAN BURIED CONCRETE SLAB
SEE STRUCTURE PLANS

WETLAND BOUNDARY
103+29.05, -28.41' LT
51-FT CPCS 18-INCH
DISCHARGE ELEV = 1041.93
PIPE SLOPE 14.03%

TIMBER COULEE CREEK

RIPRAP HEAVY
W/ GEOTEXTILE TYPE HR
(TYP)

PC: 102+82.99

R19.7'

102+83.43, 29.00' RT

STA 102+63.44,30.28' RT

RADIUS POINT

STA 103+15.99,39.48' RT

VERNON ELECTRIC COOP

VERNON COMMUNICATIONS COOP

STA 103+45.50,15.59' RT

STA 103+38.50,25.96' RT
INLETS MEDIAN 1 GRATE WITH INLET
COVERS TYPE MS REQ'D
INLET PROTECTION TYPE D REQ'D
SLOPE 6:1 AWAY FROM PAVEMENT
EDGE TO MATCH EXISTING
ELEV = 1051.44
SEE CROSS SECTION

END CONSTRUCTION
STA 103+45.50
MATCH EXISTING
SAW CUT REQ'D

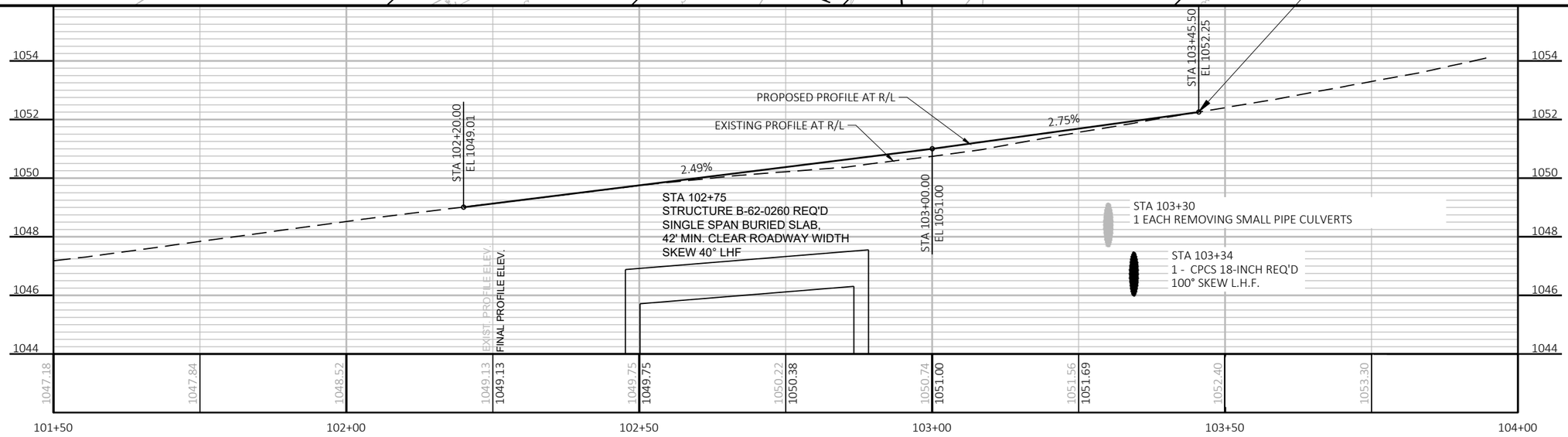
KOLBO RD

Elt: 103+94.60

Plt: 103+93.46

STATE OF WISCONSIN
DEPARTMENT OF NATURAL RESOURCES

- TOPSOIL, EROSION MAT CLASS II TYPE C
- SALVAGED TOPSOIL, EROSION MAT CLASS II TYPE C
- SILT FENCE



PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	PLAN AND PROFILE: CTH P	SHEET 19	E

FILE NAME : \\KLENGINEERING\DATA1\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - PP1

PLOT DATE : 7/8/2020 10:01 AM PLOT BY : JON BLOMQUIST PLOT NAME : PLOT SCALE : 1 IN:20 FT

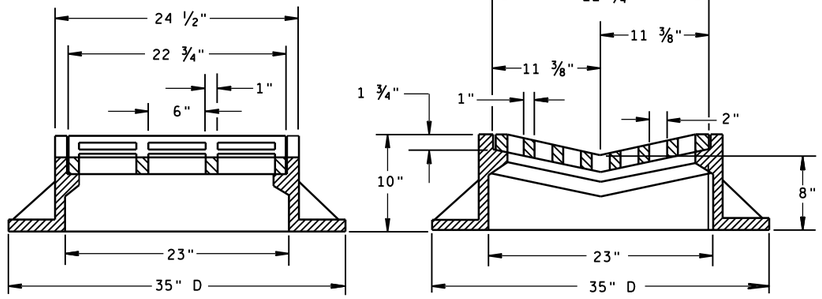
WISDOT/CADDS SHEET 44

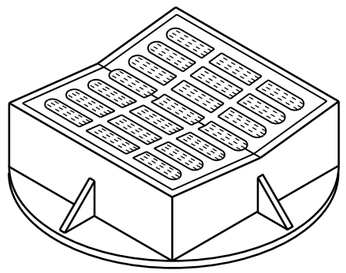
Standard Detail Drawing List

08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08C08-02	INLETS MEDIAN 1 AND 2 GRATE
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
12A03-10	NAME PLATE (STRUCTURES)
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS

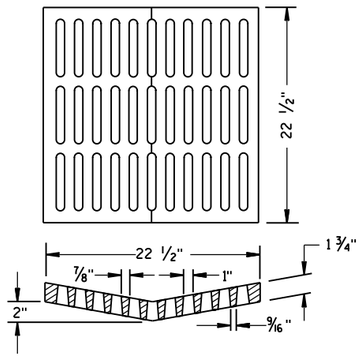
6

6



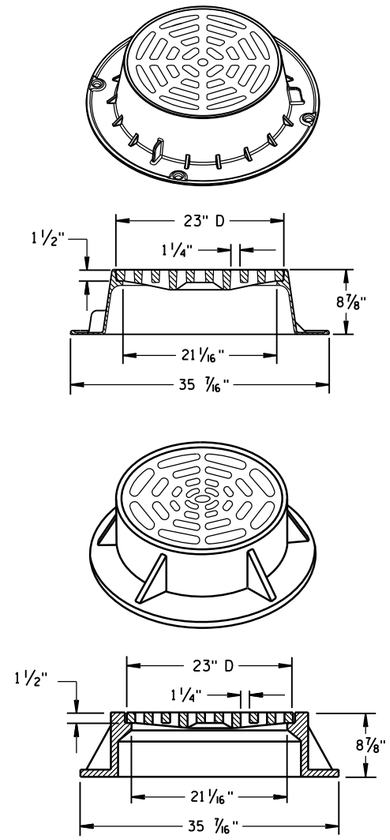


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

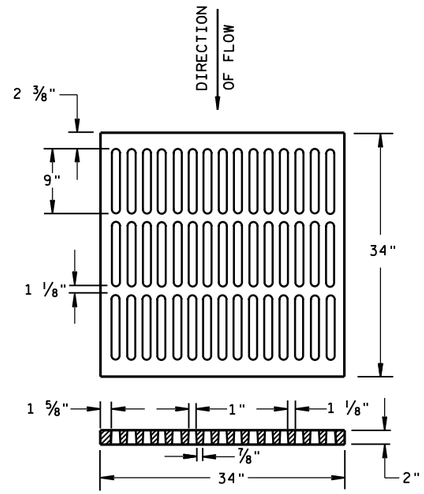
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

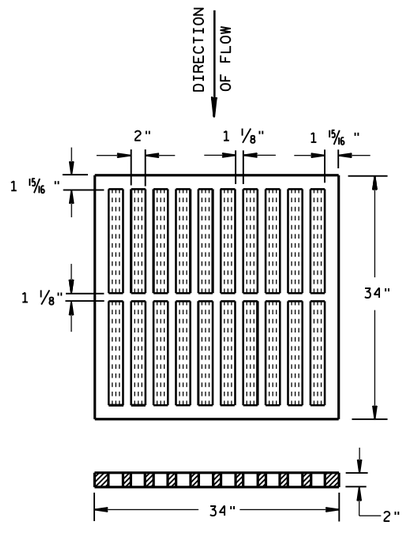
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



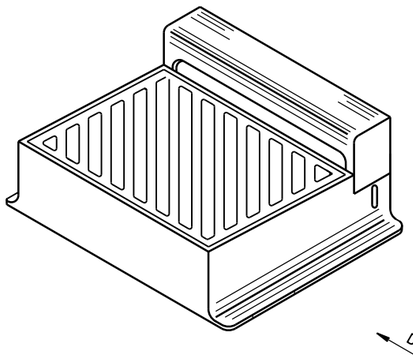
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

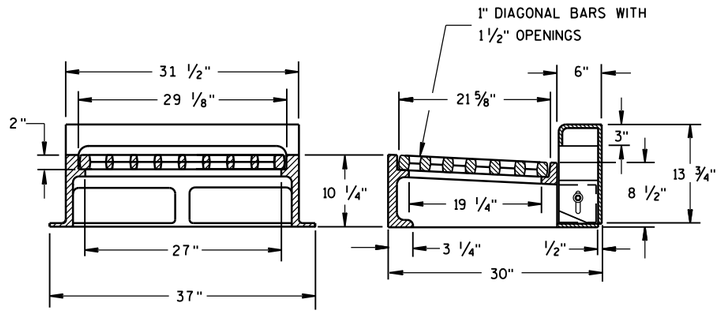


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

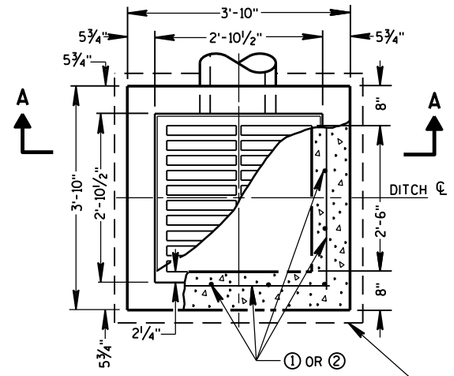
INLET COVERS
TYPE B, B-A, C, MS, MS-A, & WM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

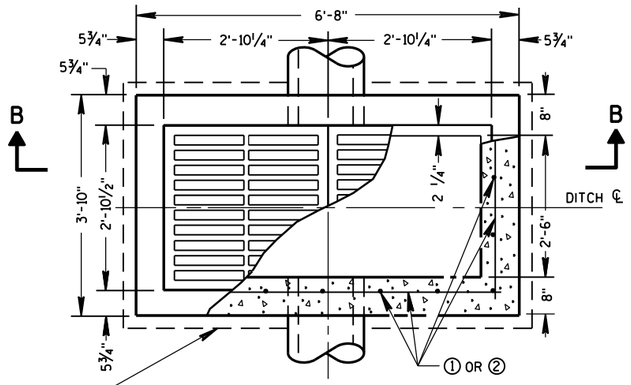
APPROVED
11/27/2013 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 8 A 5-19b

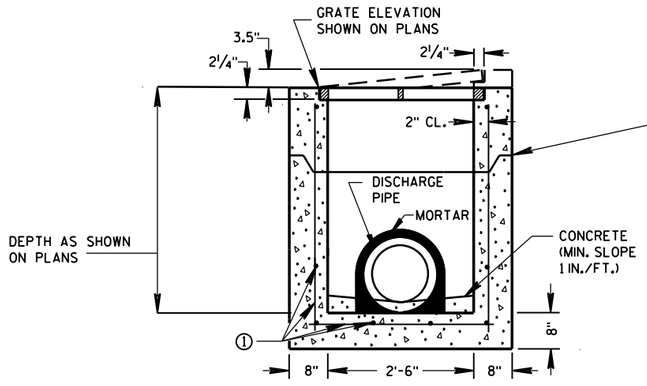


PLAN VIEW

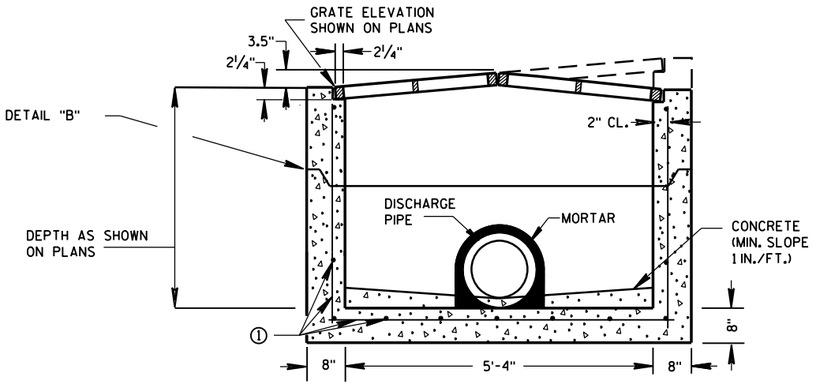


PLAN VIEW

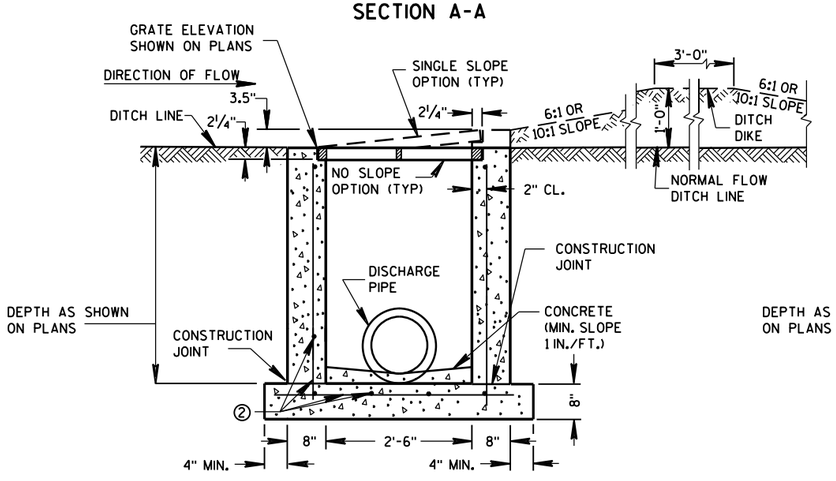
4" OVERHANGING BASE ON REINFORCED CAST-IN-PLACE CONCRETE INLETS



PRECAST REINFORCED CONCRETE SECTION A-A

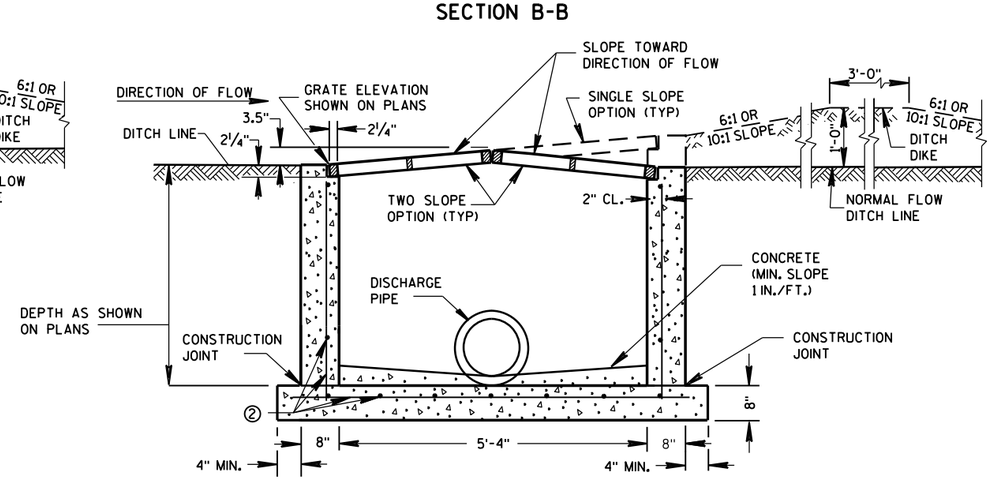


PRECAST REINFORCED CONCRETE SECTION B-B



REINFORCED CAST-IN-PLACE CONCRETE SECTION A-A

INLETS MEDIAN 1 GRATE



REINFORCED CAST-IN-PLACE CONCRETE SECTION B-B

INLETS MEDIAN 2 GRATE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, IG-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

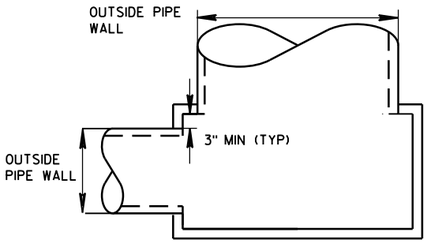
MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

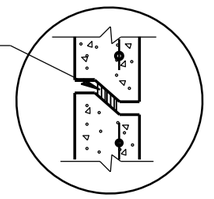
PIPE MATRIX

	MAXIMUM INSIDE PIPE DIAMETER	
INLET SIZE	WIDTH (IN)	LENGTH (IN)
1 GRATE	18	18
2 GRATE	18	42



DETAIL "A"

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



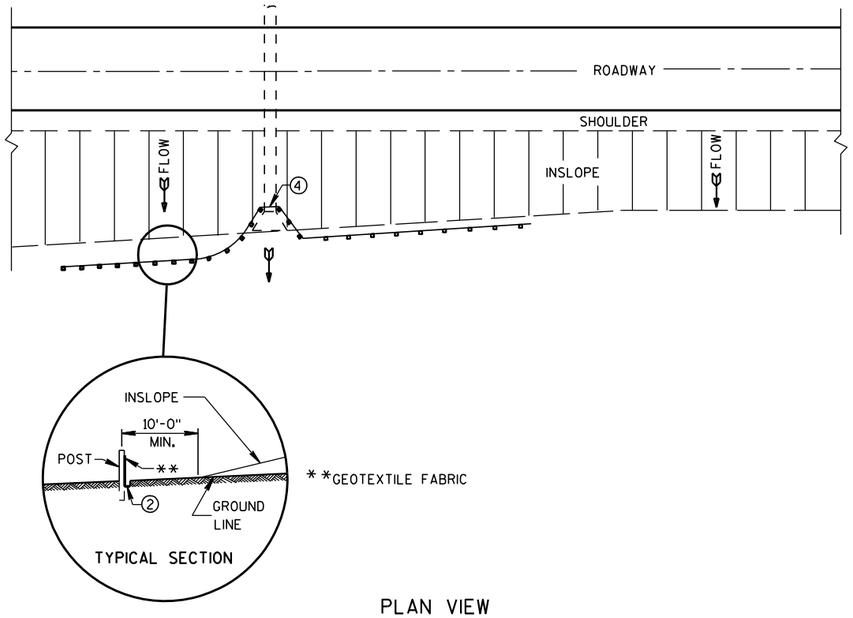
DETAIL "B"

INLETS MEDIAN 1 AND 2 GRATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

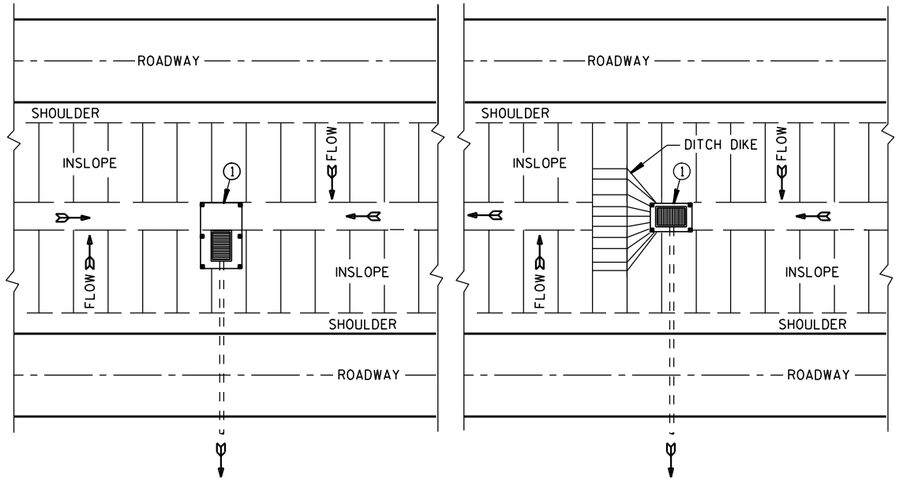
APPROVED
Sept., 2016 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS UNIT SUPERVI ENT
FHWA

6
S.D.D. 8 C 8-2



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

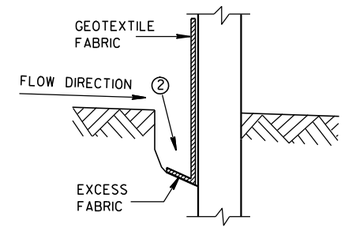
① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.

② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

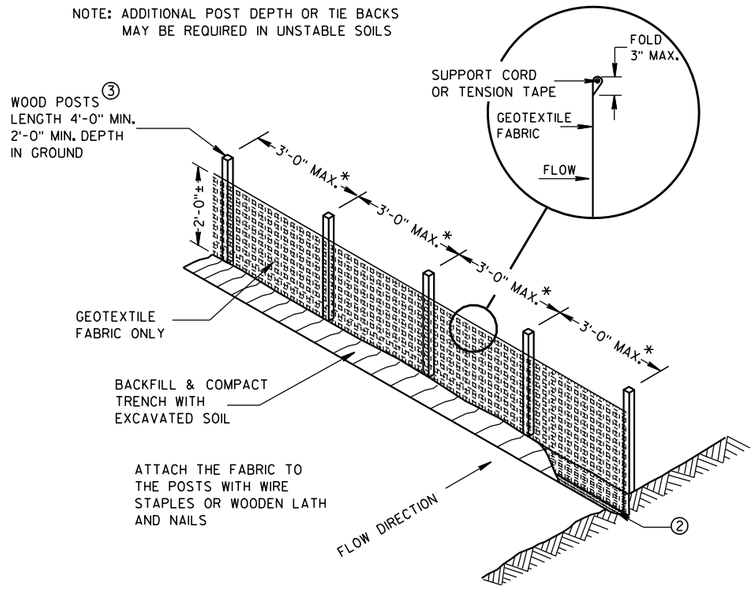
③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.

④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.

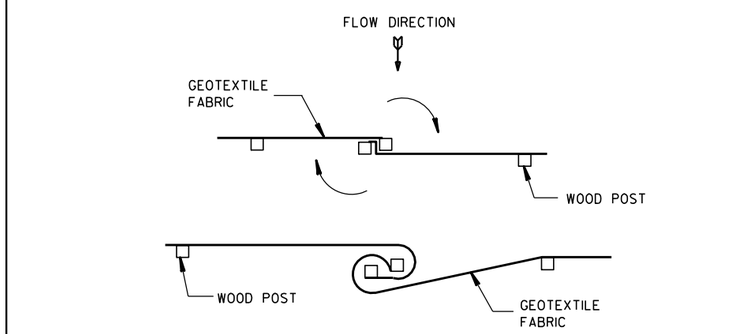
⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



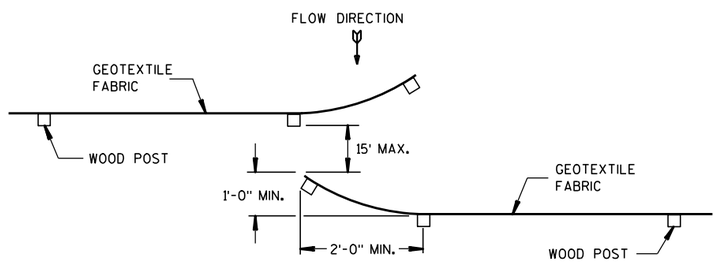
TRENCH DETAIL



SILT FENCE

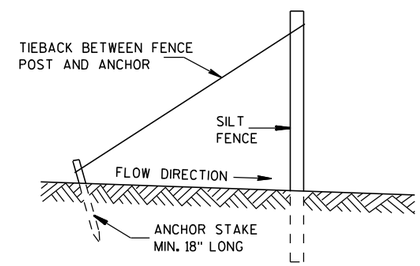


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

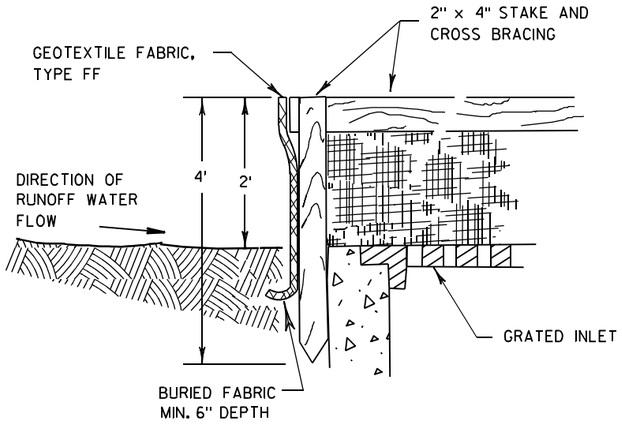
APPROVED
4-29-05
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

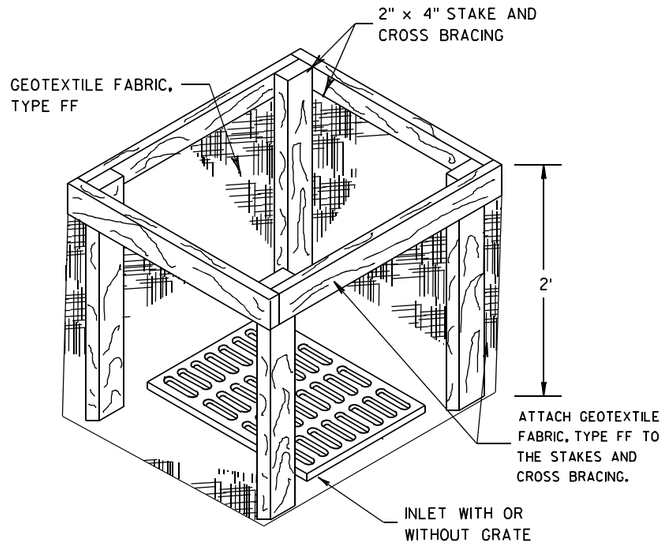
6

S.D.D. 8 E 9-6

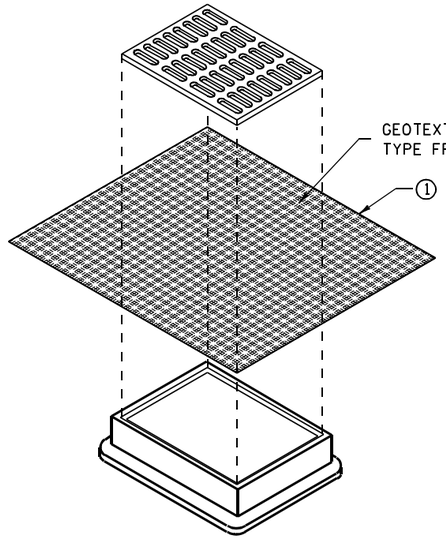
6

S.D.D. 8 E 9-6



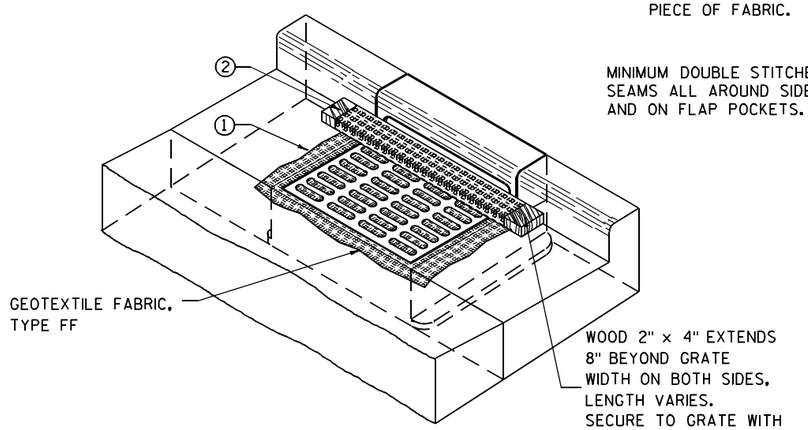


INLET PROTECTION, TYPE A

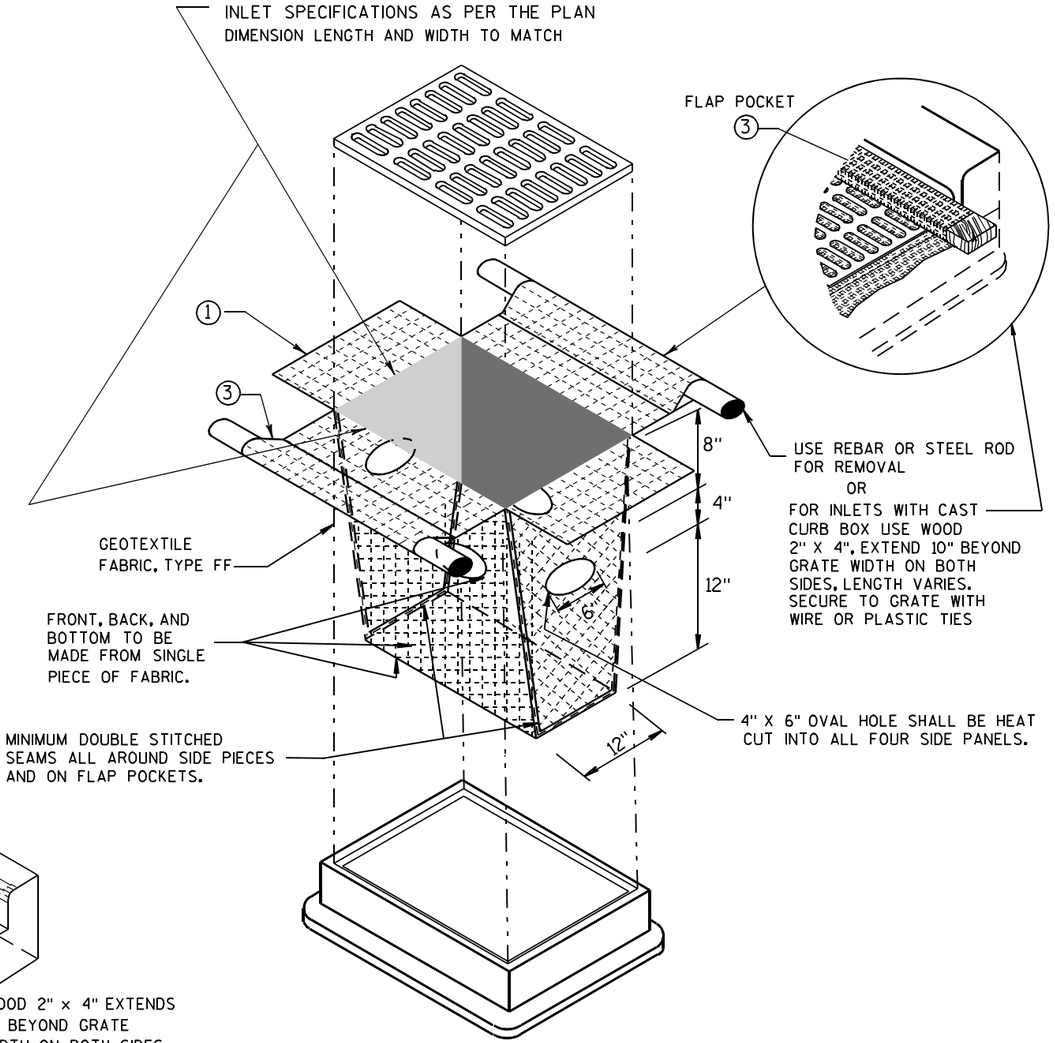


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

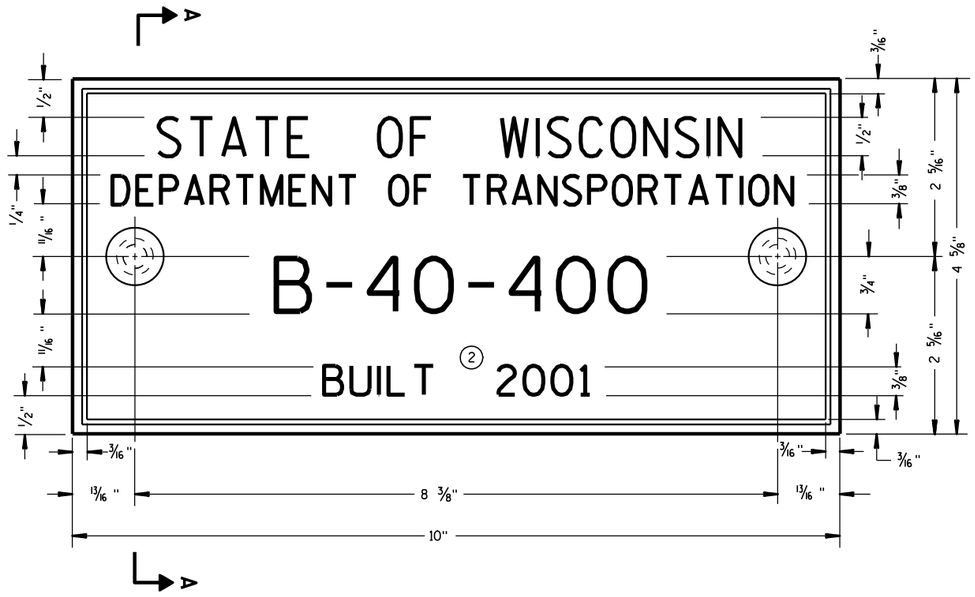
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6
S.D.D. 8 E 10-2



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

FOR MULTI-UNIT STRUCTURES
LINE 3 ABOVE SHALL READ

B = BRIDGE
C = CULVERT
R = RETAINING WALL

COUNTY NO.

BRIDGE NO.

UNIT NO. FOR MULTIPLE UNIT BRIDGE

B-40-400-1A

NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES

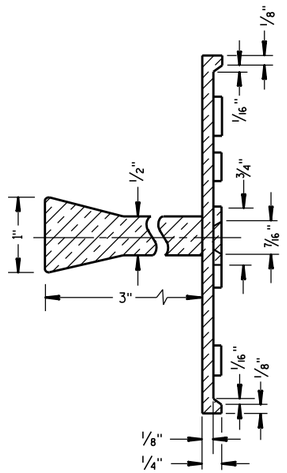
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

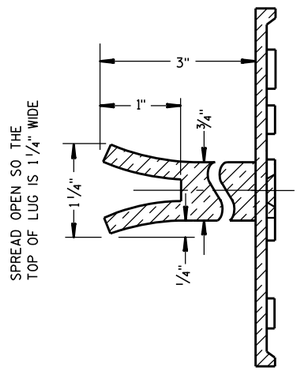
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

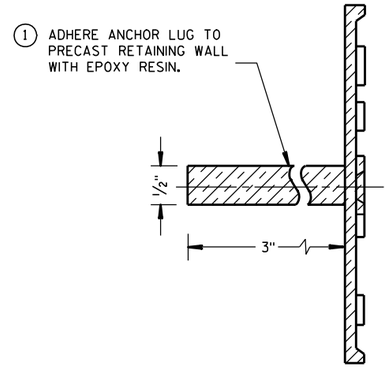
(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A



ALTERNATE LUG



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

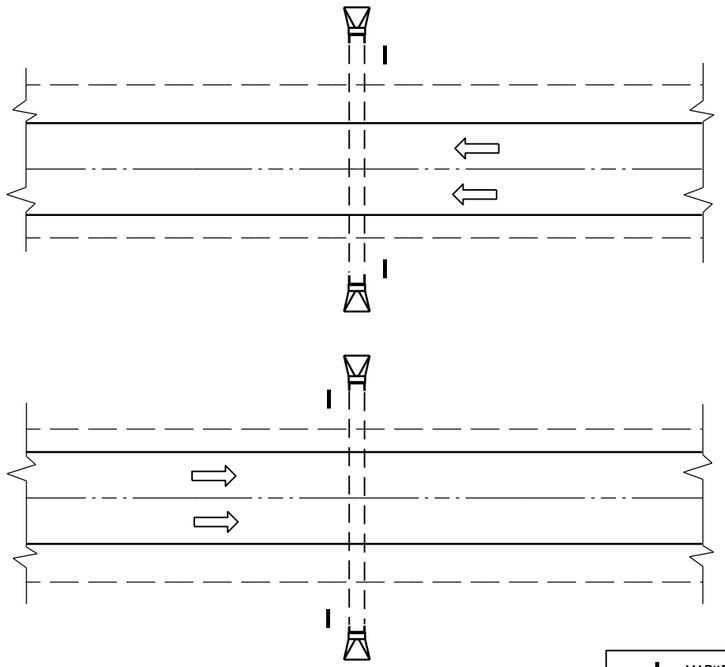
3/26/10 DATE

/S/ Scot Beck -- CHIEF STRUCTURAL DEVELOP ENGINEER

FHWA

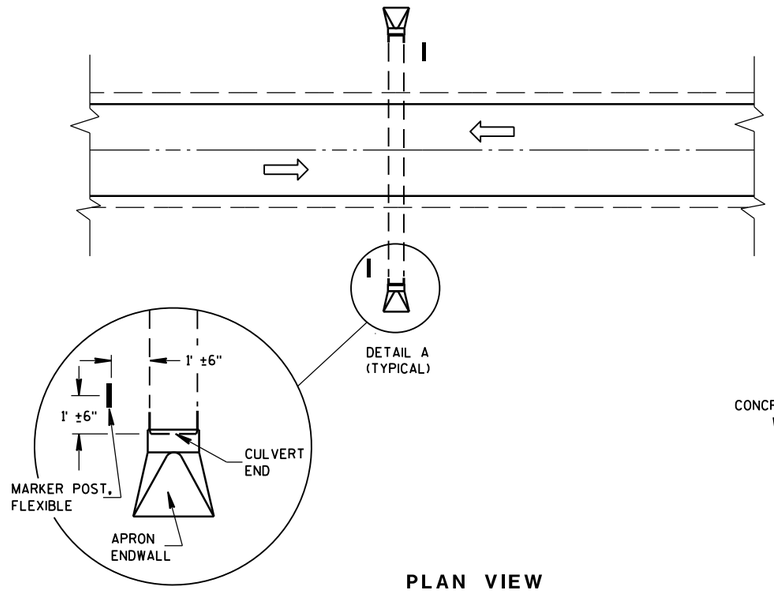
6

S.D.D. 12 A 3-10



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

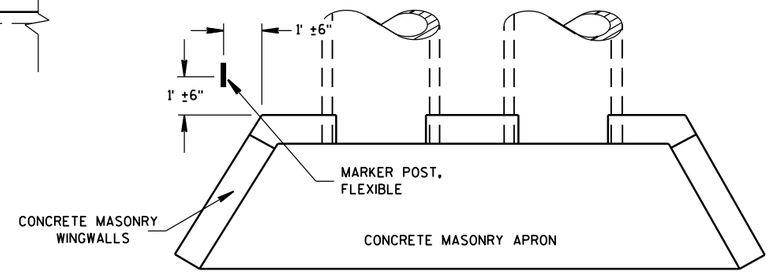
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

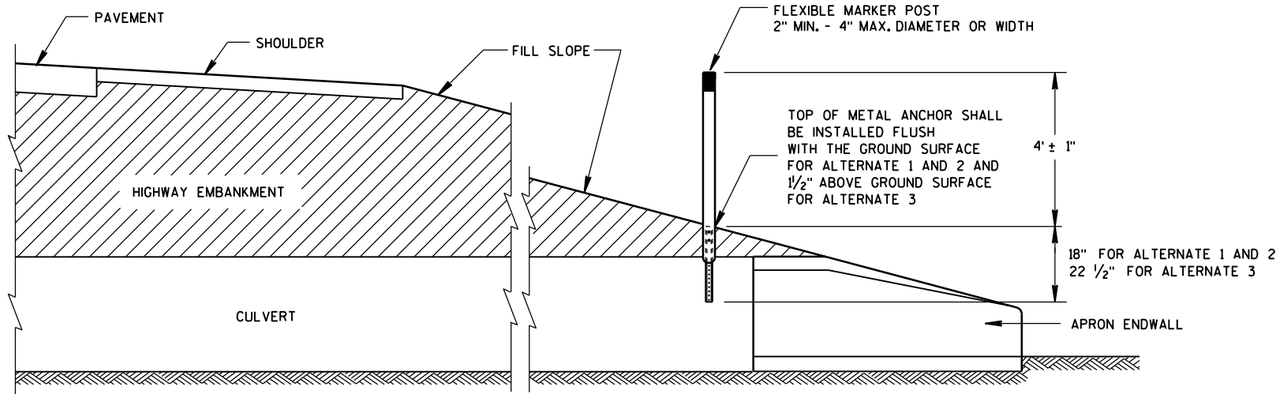
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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S.D.D. 15 A 3-2a

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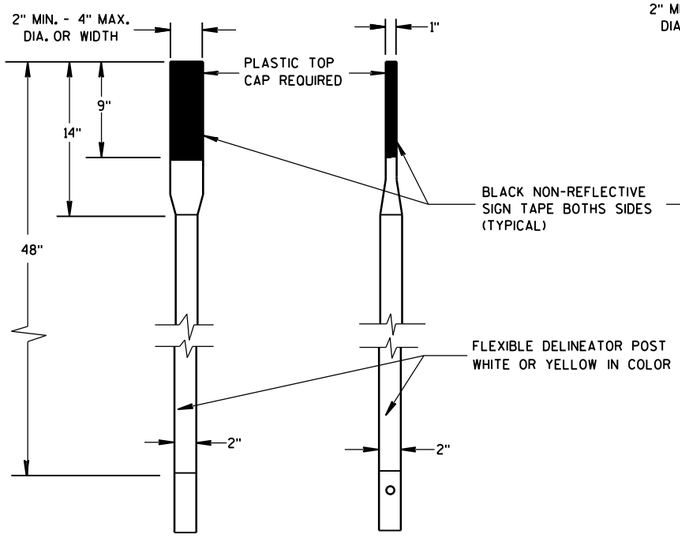
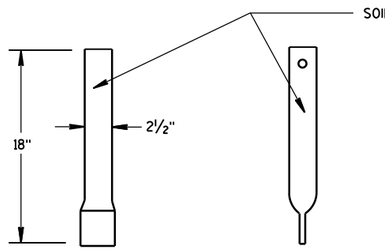
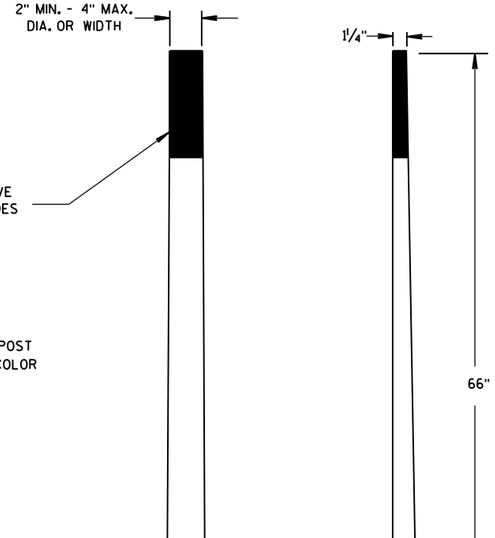
S.D.D. 15 A 3-2a





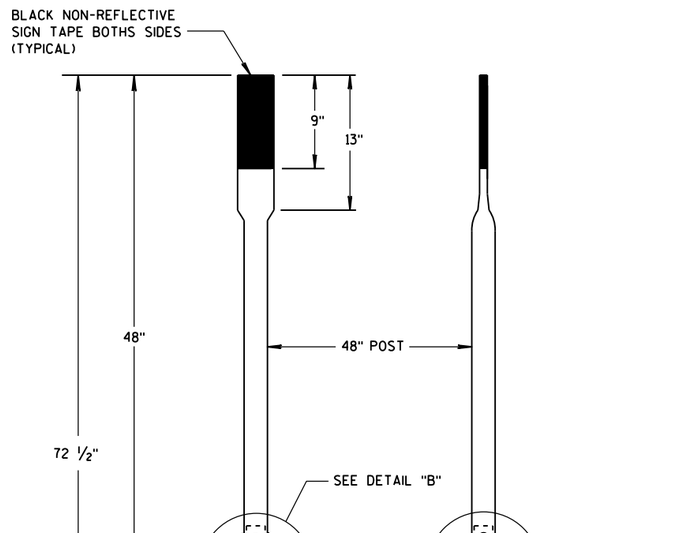
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

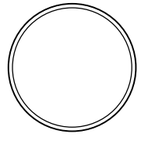
ALTERNATE 2



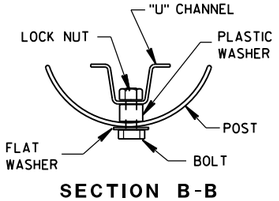
FRONT VIEW SIDE VIEW

ALTERNATE 3

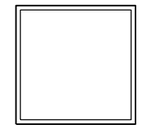
FLEXIBLE MARKER POSTS



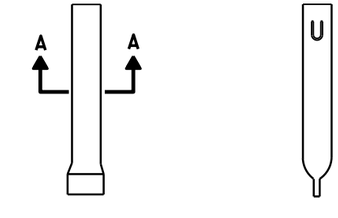
SECTION A-A



SECTION B-B

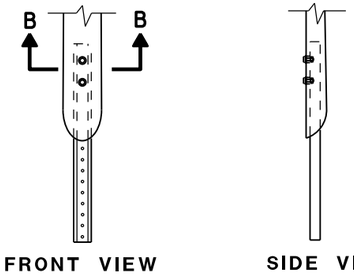


SECTION C-C



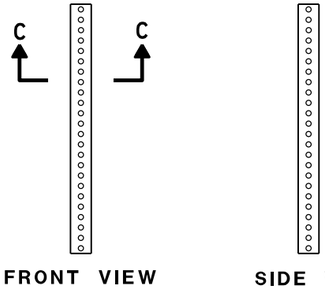
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

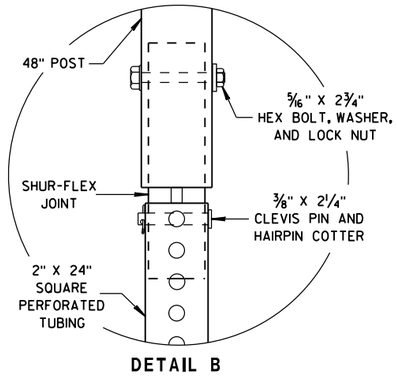
ALTERNATE 2



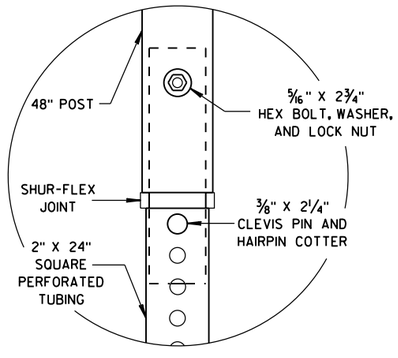
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS



DETAIL B



DETAIL C

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 A 3-2b

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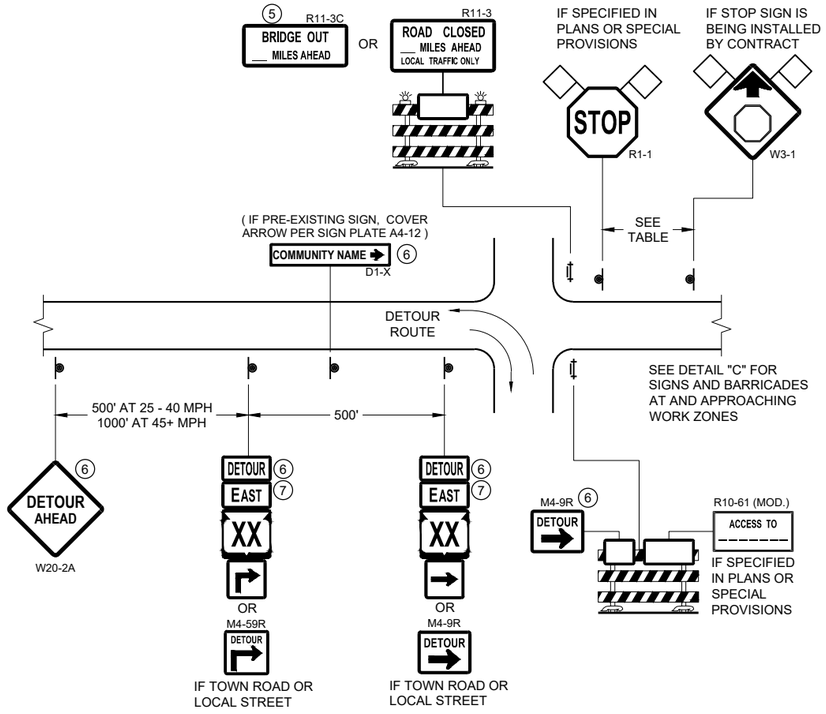
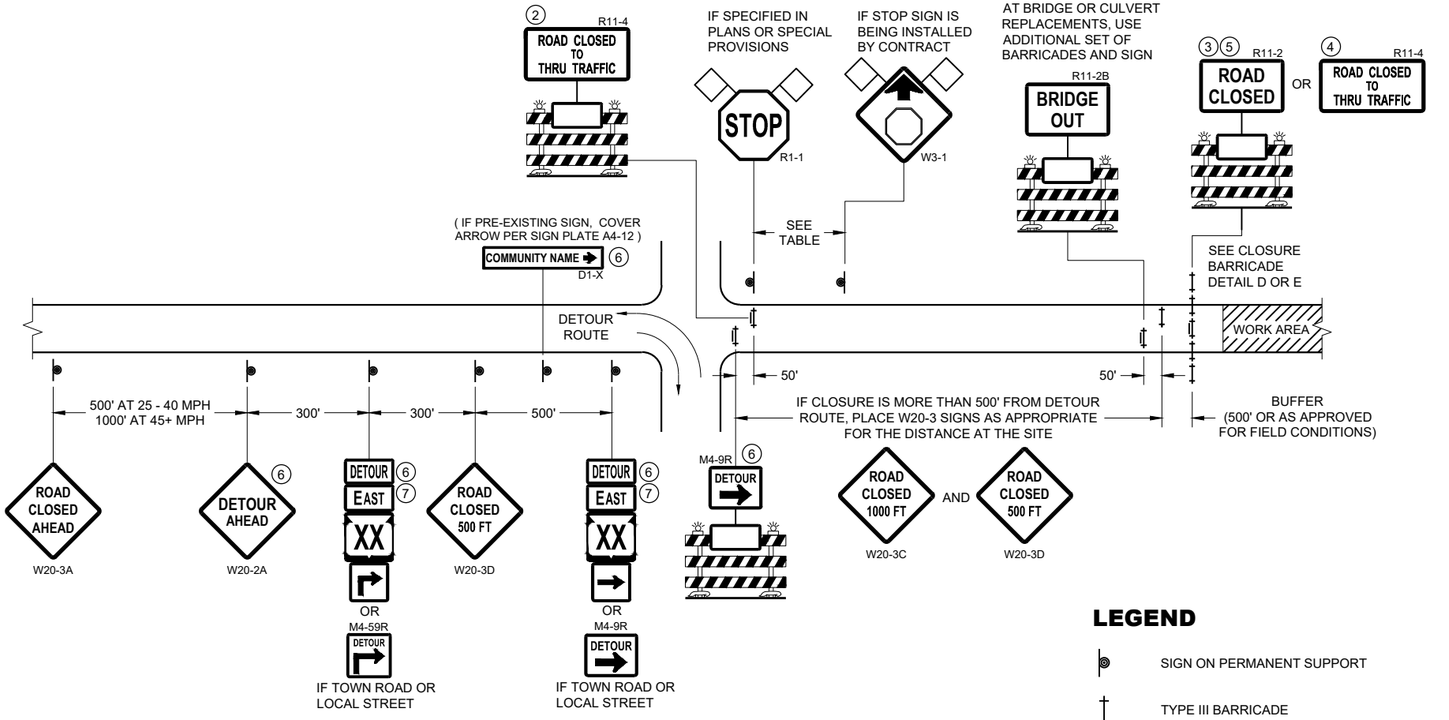
S.D.D. 15 A 3-2b



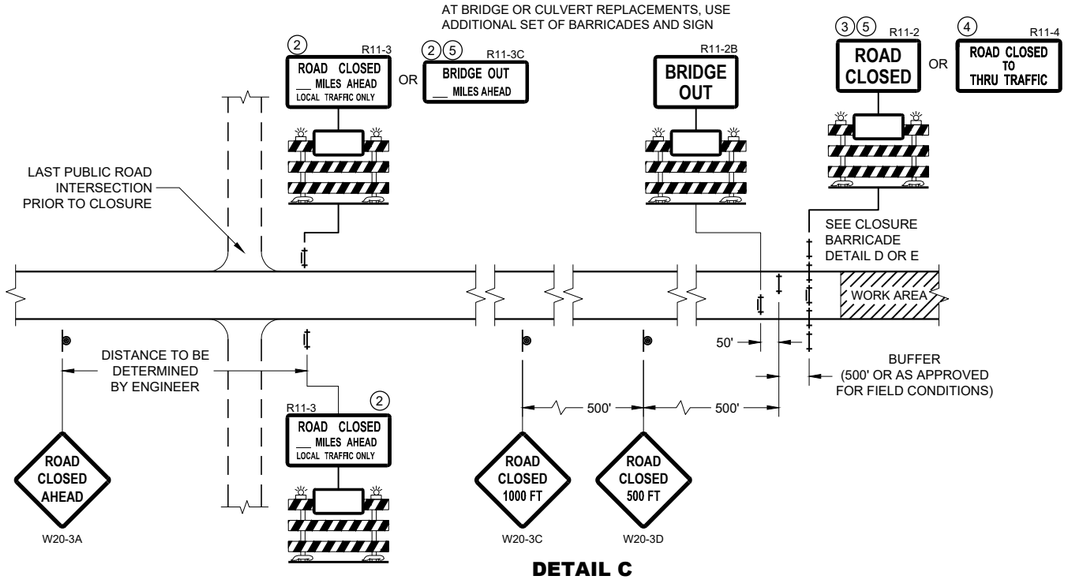


DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

DETOUR M4 - 8
EAST M3 - X
XX M1 - 4 OR XX M1 - 6 OR COUNTY X M1 - 5A
M05 - 1 OR M06 - 1

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

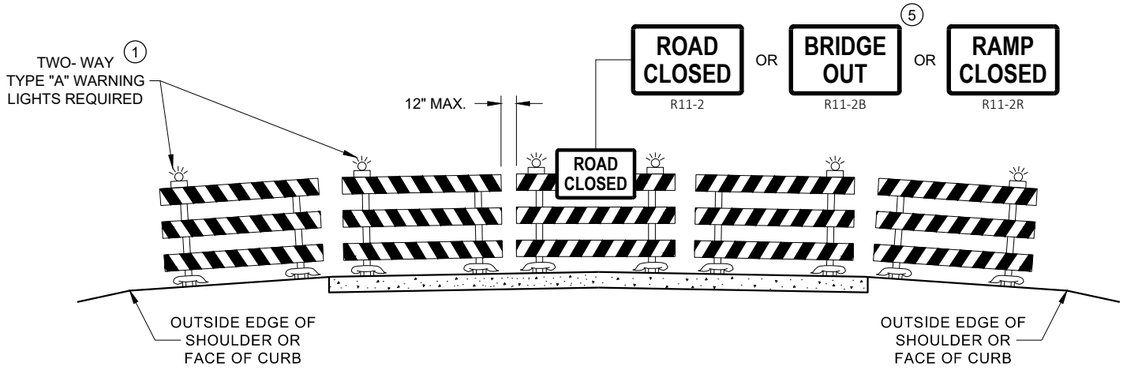
APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

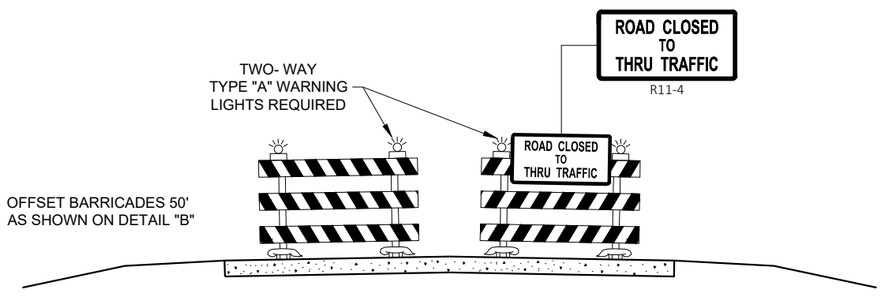
6

SDD 15C02 - 08a

SDD 15C02 - 08a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

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SDD 15C02 - 08b

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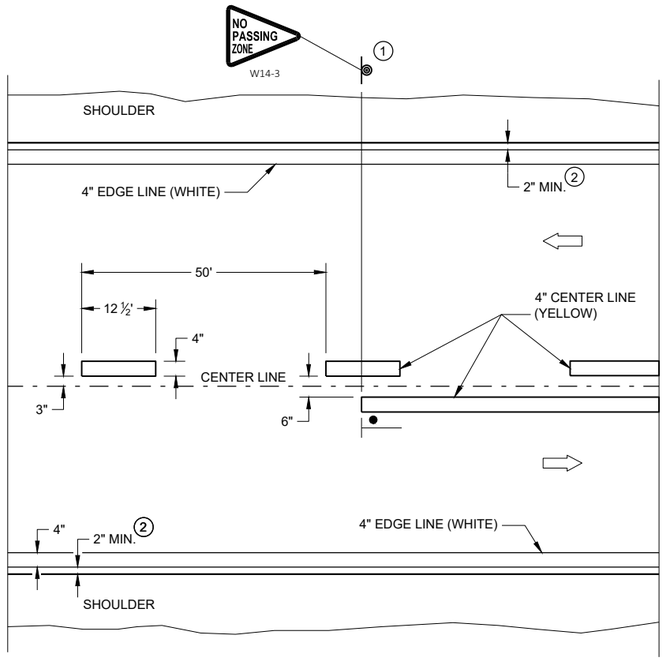
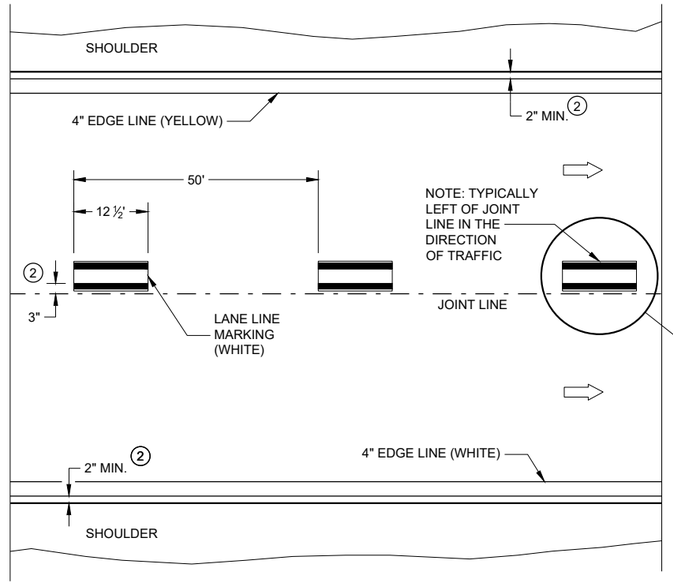
SDD 15C02 - 08b

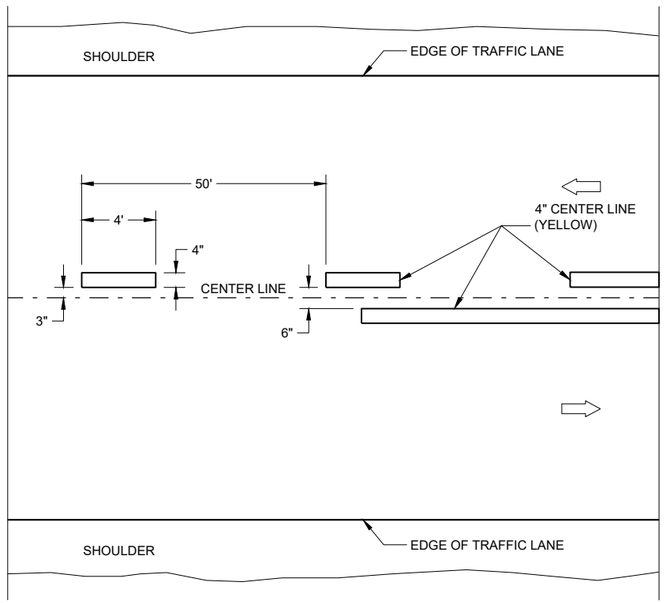


TWO WAY TRAFFIC

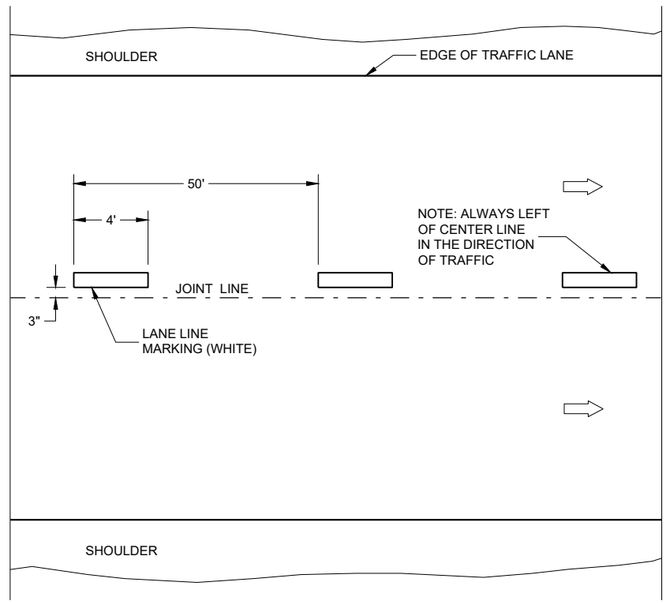


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NO SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

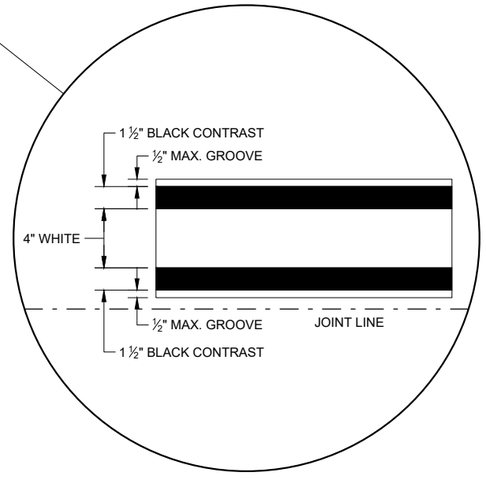
1. LOCATE THE NO PASSING ZONE W14-3 SIGN WITH 50 FEET OF THE "T" MARKING
2. MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

"T" MARKING

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC



LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Matthew Rauch
STATEWIDE SIGNING AND M ENGINEER

FHWA

SDD 15C08 - 20a

SDD 15C08 - 20a

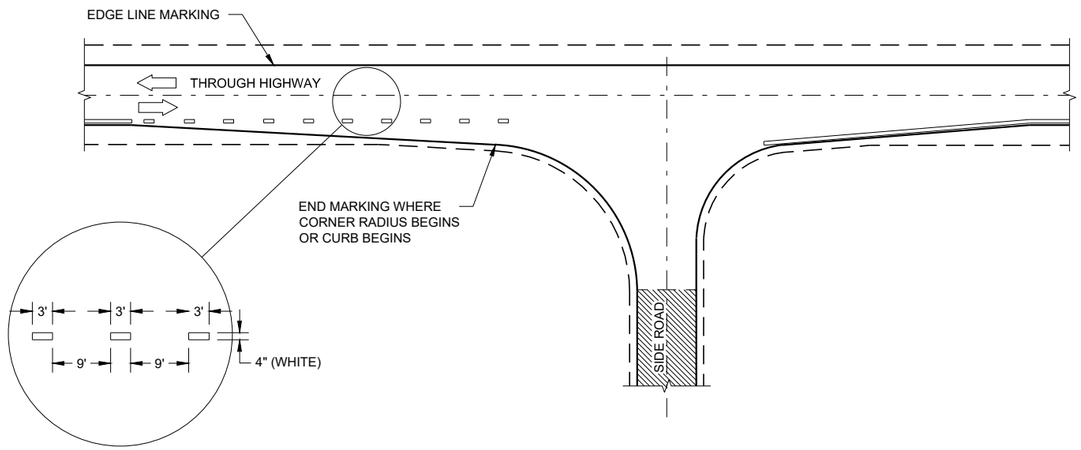
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

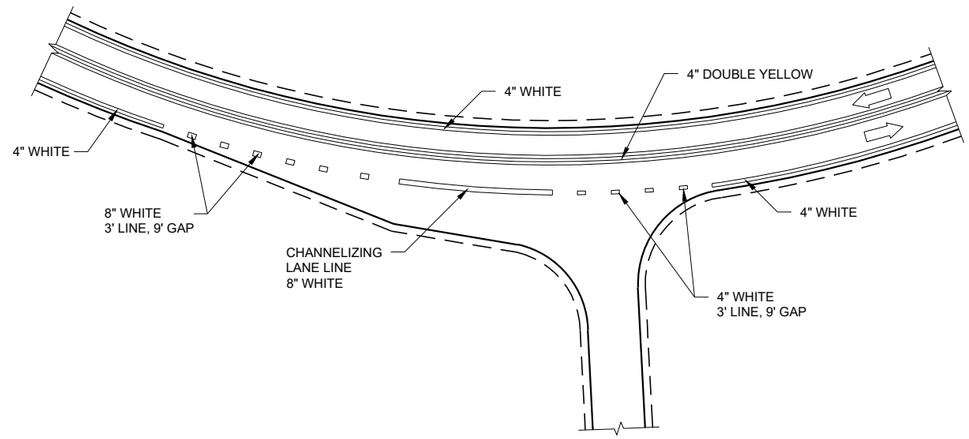
1. WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
2. WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
3. BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
4. BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

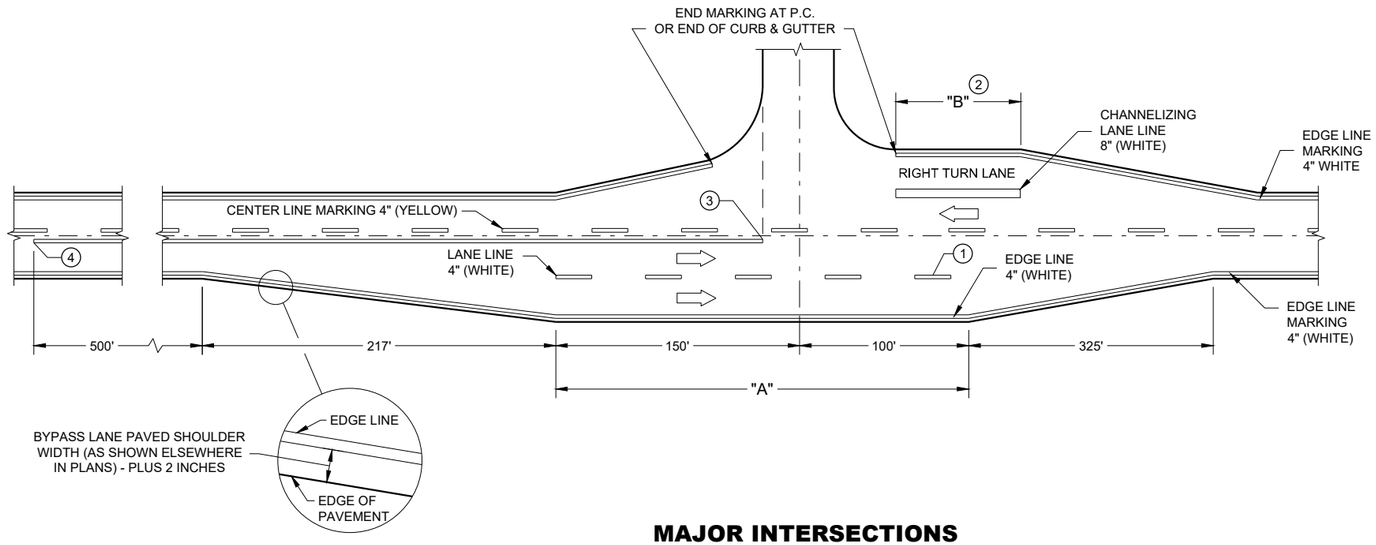
DIRECTION OF TRAVEL



MINOR INTERSECTION



INTERSECTION ON OUTSIDE OF CURVE



MAJOR INTERSECTIONS

(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

PAVEMENT MARKING (INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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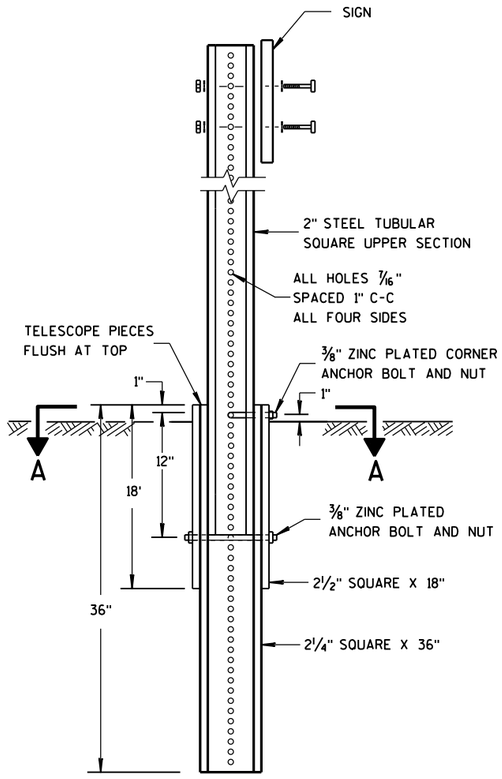
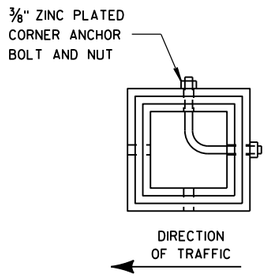
SDD 15C35 - 04a

6

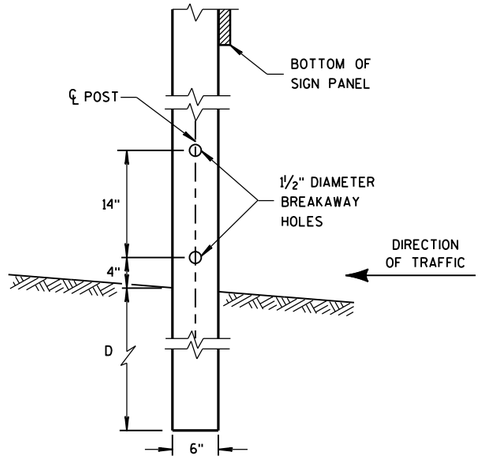
SDD 15C35 - 04a



DETAIL OF TUBULAR STEEL SIGN POST



SECTION A-A



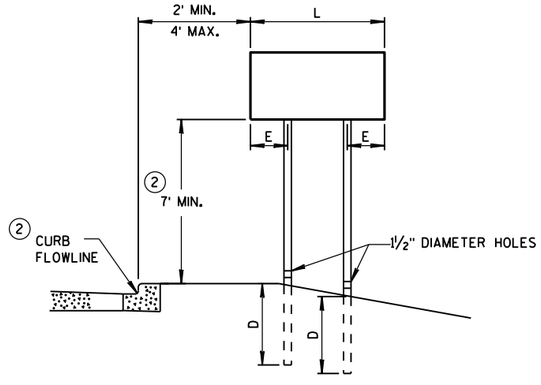
4"x6" WOOD POST MODIFICATION

GENERAL NOTES

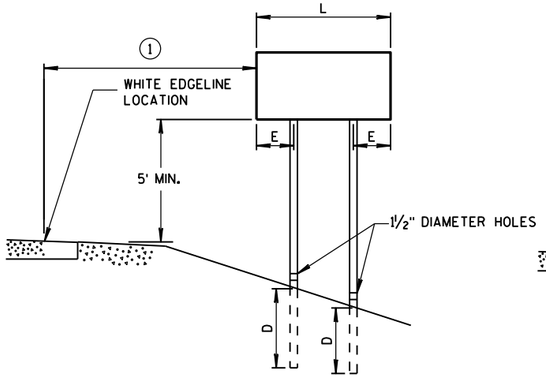
(1) 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.

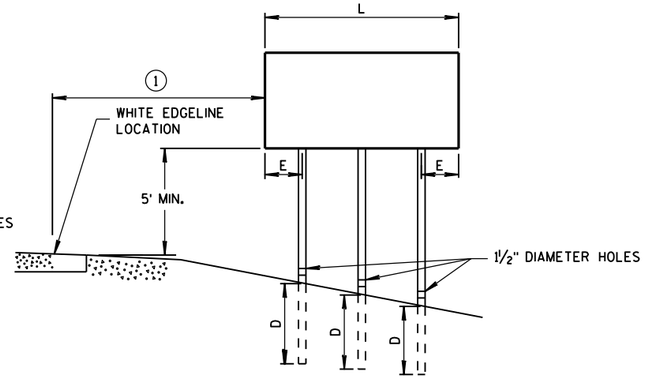
(2) THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.

(3) FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.



URBAN AREA





RURAL AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

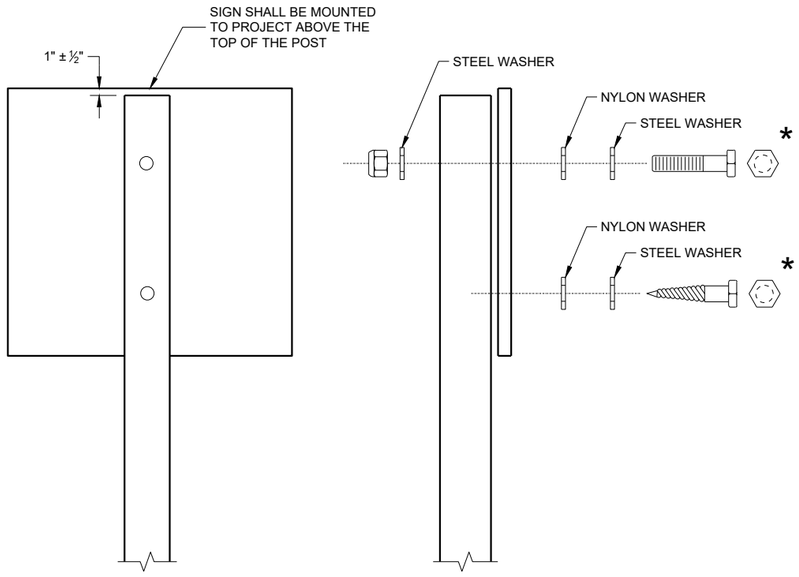
SEE NOTE (3)

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

6

S.D.D. 15 D 38-2a
S.D.D. 15 D 38-2a



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POST (4" x 6")
LAG SCREWS - ⅜" x 3"
MACHINE BOLTS - 5/16" x 6 ½" OR 7" LENGTH W/NUTS

SQUARE STEEL POST (2" x 2")
MACHINE BOLTS - ⅜" x 3 ¼" LENGTH W/NUTS
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE 0.720 - 0.765 INCH, GRIP RANGE 0.042 - 0.375 INCH

WASHERS (ALL POSTS) -
1 ¼" O.D. x ⅜" I.D. x 1/16" STEEL
1 ¼" O.D. x ⅜" I.D. x 0.080 NYLON

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

6

6

SDD 15D38 - 02b

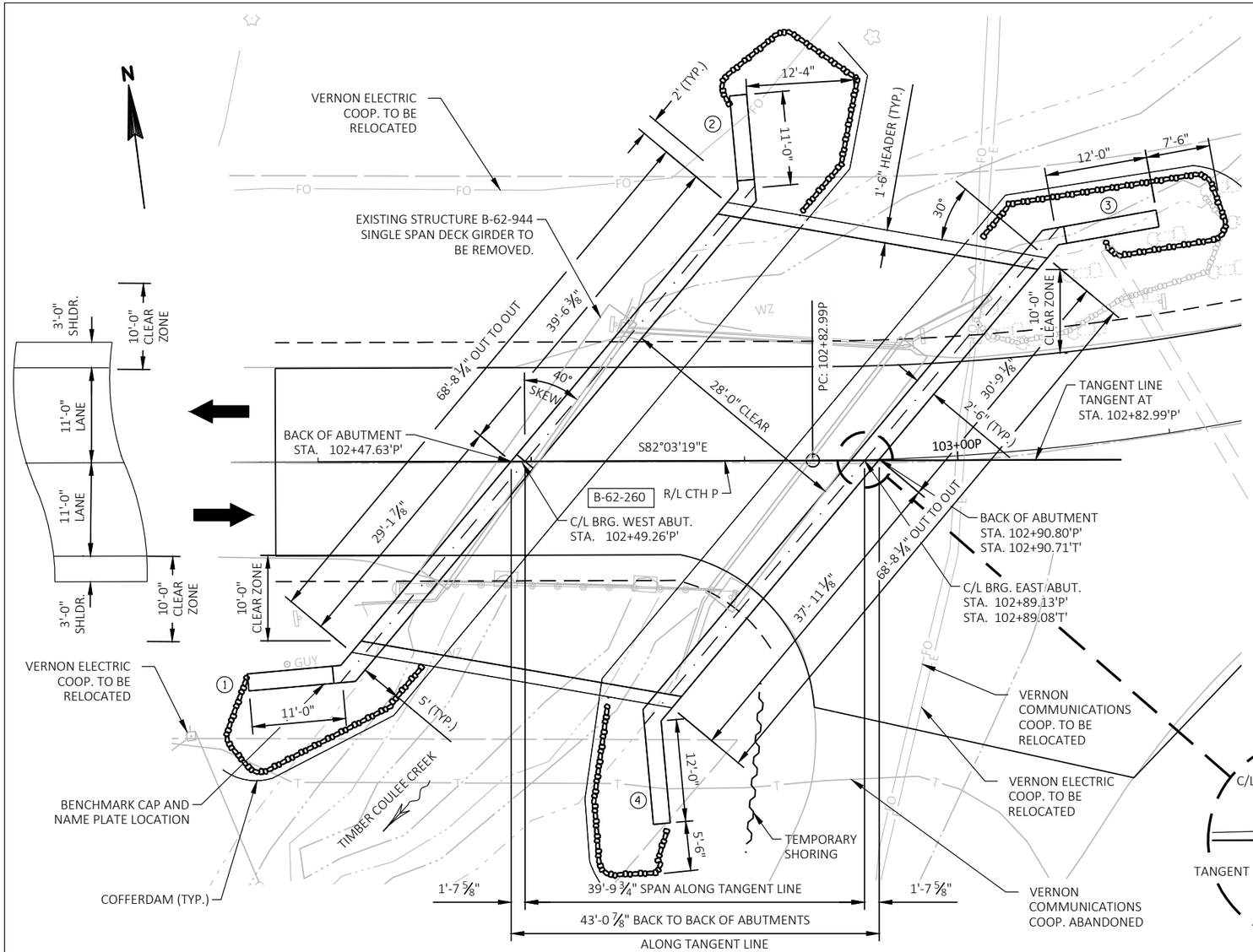
SDD 15D38 - 02b

ATTACHMENT OF SIGNS TO POSTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

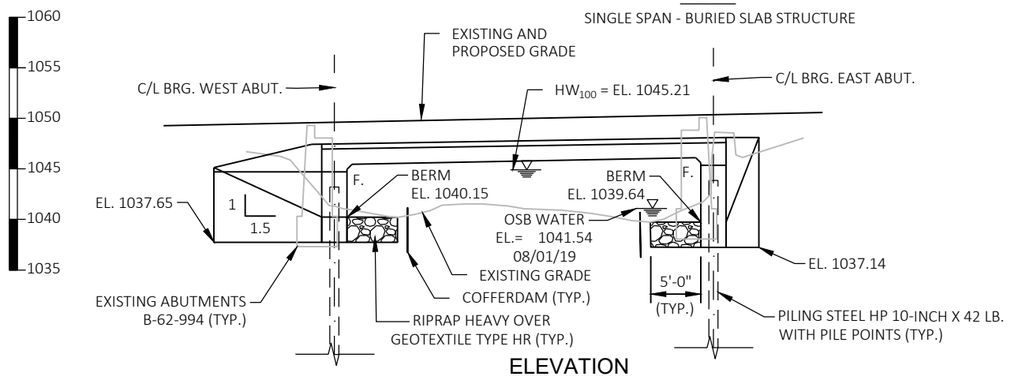
APPROVED
June 2017
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE
FHWA

STATE PROJECT NUMBER
5478-00-72



PLAN

SINGLE SPAN - BURIED SLAB STRUCTURE



ELEVATION

NORMAL TO TIMBER COULEE CREEK

PI STA = 103+38.94
Y = 196852.221
X = 712363.923
DELTA = 22°27'10"
D = 20°19'25"
T = 55.96'
L = 110.48'
R = 281.92'
PC STA = 102+82.99
Y = 196859.955
X = 712308.503
PT STA = 103+93.46
Y = 196866.239
X = 712418.095
BK = S82°03'18.9"E
AH = N75°29'30.8"E

DESIGN DATA

LIVE LOAD:
DESIGN LOADING: HL-93
INVENTORY RATING FACTOR: RF=1.38
OPERATING RATING FACTOR: RF=1.79
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV) = 250 KIPS

EARTHLOAD:
DESIGNED FOR FILL HEIGHT RANGE OF 2.0 TO 4.0 FT.

MATERIAL PROPERTIES:
CONCRETE MASONRY BRIDGES SUPERSTRUCTURE.....f'c = 4,000 P.S.I.
CONCRETE MASONRY BRIDGES SUBSTRUCTURE.....f'c = 3,500 P.S.I.
BAR STEEL REINFORCEMENT HIGH STRENGTH, GRADE 60.....f'c = 60,000 P.S.I.

TRAFFIC VOLUME

CTH P
A.A.D.T. = 190 (2019)
A.A.D.T. = 237 (2041)
RDS = 40 MPH

HYDRAULIC DATA

100 YEAR FREQUENCY

Q_{100} = 720 C.F.S.
VEL. = 6.43 F.P.S.
H.W. = ELEV. 1045.21
WATERWAY AREA = 112 SQ. FT.
DRAINAGE AREA = 1.52 SQ. MI.
ROAD OVERTOPPING = N/A
SCOUR CRITICAL CODE = 5

2 YEAR FREQUENCY

Q_2 = 140 C.F.S.
$H.W._2$ ELEV. = 1042.46
VEL. = 3.40 FT/S

LEGEND

(#) INDICATES WING NUMBER

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON PILING STEEL HP 10-INCH X 42 LB. WITH PILE POINTS DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 180 TONS** PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 30-FT LONG AT WEST ABUTMENT AND 20-FT LONG AT EAST ABUTMENT.

** THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.50 USING MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

LIST OF DRAWINGS

WISCONSIN
CHAD D. HALVERSON
E-34364
MADISON, WI
PROFESSIONAL ENGINEER

July 7, 2020

STRUCTURE DESIGN CONTACTS
BUREAU OF STRUCTURES:
AARON BONK (608) 261-0261
CONSULTANT:
CHAD HALVERSON (608) 663-1218

NO.	DATE	REVISION	BY

KL Engineering [A] Better Experience

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED SDR 08/04/20
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-62-260

CTH P OVER TIMBER COULEE CREEK

COUNTY	TOWN ~~/CITY/VILLAGE~~
VERNON	CHRISTIANA

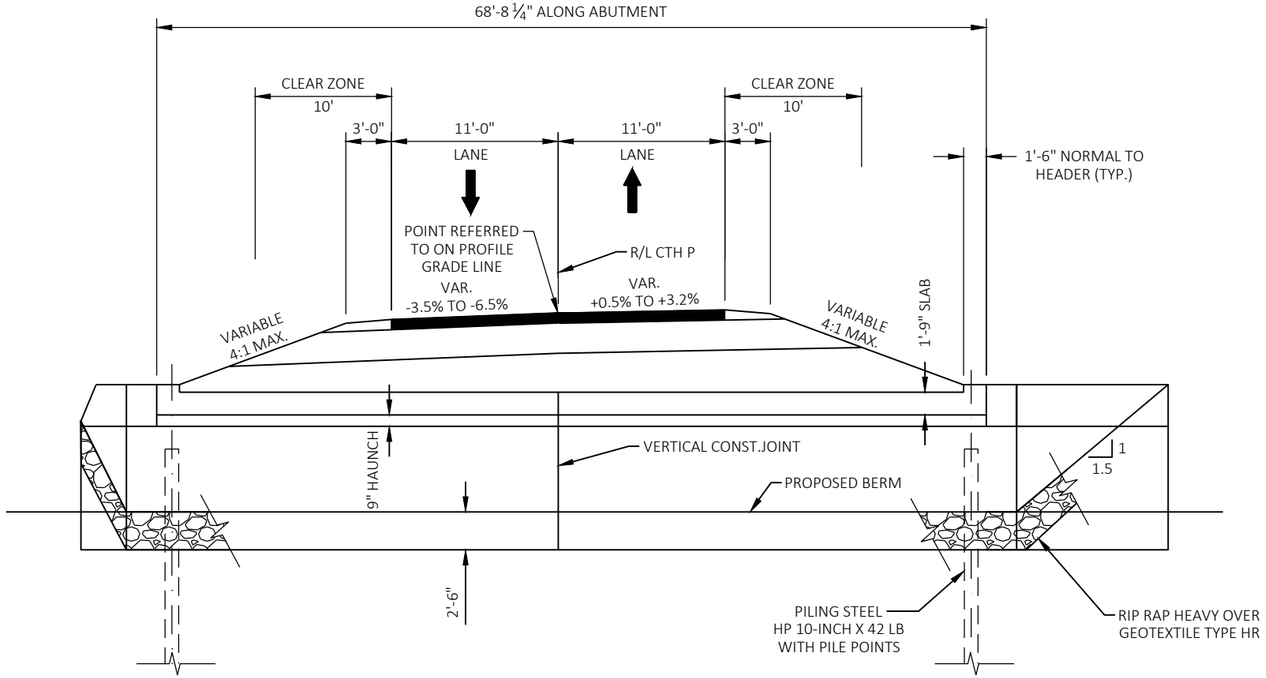
DESIGN SPEC.
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS

DESIGNED BY	CDH	DESIGN CK'D.	CAH	DRAWN BY	STD	PLANS CK'D.	CAH

GENERAL PLAN

SHEET 1 OF 11
34

STATE PROJECT NUMBER
5478-00-72



PROPOSED CROSS SECTION

(LOOKING EAST)

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

AT THE BACKFACE OF ABUTMENT ALL VOLUME (WITHIN THE PAY LIMITS SHOWN BELOW) WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURAL BACKFILL.

THE BACKFILL QUANTITIES ARE BASED ON THE PAY LIMITS SHOWN ON THE PLANS AND MAY NOT REFLECT ACTUAL PLACED QUANTITIES. "BACKFILL STRUCTURE TYPE A" REQUIRED DIRECTLY BEHIND ABUTMENTS AND ABUTMENT WINGS FOR 3 FEET. BACKFILL PLACED BEYOND PAY LIMITS OR EXCEEDING PLAN QUANTITIES SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES.

THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE 'HR' TO THE EXTENT SHOWN ON SHEET 1 AND IN THE ABUTMENT DETAILS.

THE EXISTING STRUCTURE B-62-994, IS A SINGLE SPAN DECK GIRDER WITH A WIDTH OF 32 FT AND A LENGTH OF 35 FT. EXISTING SUPERSTRUCTURE AND FULL RETAINING ABUTMENTS TO BE REMOVED.

THE EXISTING GROUND LINE SHALL BE THE UPPER LIMITS OF THE EXCAVATION FOR STRUCTURES.

PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE TOP AND VERTICAL FACE OF THE HEADERS AND OUTSIDE 1'-0" OF THE UNDERSIDE OF THE SLAB, TOP OF WINGS, TO THE EXPOSED FRONT FACES OF WINGS, AND TO THE EXPOSED ABUTMENT FACES EXTENDING TO 1'-0" IN FROM THE EDGE OF SLAB.

PLACE SHEET MEMBRANE WATERPROOFING ON ENTIRE TOP SLAB, BETWEEN HEADERS, AND EXTEND DOWN 1'-0" AT ABUTMENT ENDS AS SHOWN ON SUPERSTRUCTURE DETAILS 1 SHEET.

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE, UNLESS AN ALTERNATE METHOD IS APPROVED BY ENGINEER.

DO NOT PLACE FILL ABOVE 3'-0" FROM THE BOTTOM OF THE ABUTMENT UNTIL THE SUPERSTRUCTURE IS IN PLACE.

LEGEND

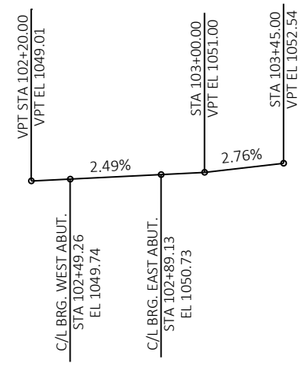
- BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.
- PIPE UNDERDRAIN WRAPPED (6-INCH), SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

TOTAL ESTIMATED QUANITIES

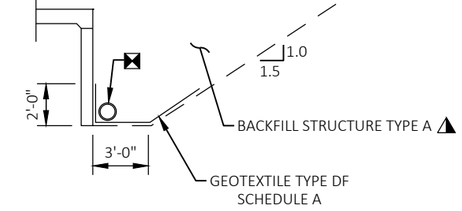
ITEM NUMBER	ITEM DESCRIPTION	UNIT	W. ABUT	E. ABUT	SUPER	TOTAL
203.0600.S	REMOVING OLD STRUCTURE OVER WATERWAY WITH MINIMAL DEBRIS 102+75	LS	---	---	---	1
206.1000	EXCAVATION FOR STRUCTURES BRIDGES B-62-260	LS	---	---	---	1
206.5000	COFFERDAM B-62-260	LS	---	---	---	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	470	610	300	1,380
502.0100	CONCRETE MASONRY BRIDGES	CY	64	75	159	298
502.3200	PROTECTIVE SURFACE TREATMENT	SY	18	21	37	76
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	4,780	4,980	---	9,760
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	1,980	2,170	38,600	42,750
511.1200	TEMPORARY SHORING B-62-260	SF	---	184	---	184
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	14	15	---	29
516.0610.S	SHEET MEMBRANE WATERPROOFING FOR TOP SLAB B-62-260	SY	---	---	254	254
550.0500	PILE POINTS	EACH	14	14	---	28
550.1100	PILING STEEL HP 10-INCH X 42 LB	LF	420	280	---	700
606.0300	RIPRAP HEAVY	CY	73	68	---	141
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	115	110	---	225
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	84	86	---	170
645.0120	GEOTEXTILE TYPE HR	SY	104	102	---	206
	NON-BID ITEMS					
	FILLER		½" & ¾"	½" & ¾"		

BENCHMARK TABLE

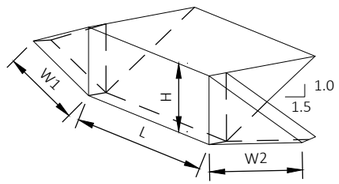
NO.	Y	X	ELEV.	DESCRIPTION
1	196,879.86	712,289.92	1,049.83	USGS DISK NW WWAL CTH P BRIDGE
2	196,703.18	712,268.81	1,060.37	RR SPIKE PPOL #4F16R24
3	196,845.34	712,293.62	1,050.02	CUT SQ SE CORNER CTH P BRIDGE



PROFILE GRADE LINE R/L CTH P



STRUCTURAL BACKFILL DETAIL



ABUTMENT BACKFILL DIAGRAM

L = OUT TO OUT OF abutment body (FT)
H = AVERAGE abutment fill height (FT)
W1 = WING 1 LENGTH (FT)
W2 = WING 2 LENGTH (FT)
EF = Expansion Factor (1.20 for CY bid items and 1.00 for TON bid items)
V_{cf} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3.0')(0.5)(W1+W2)(H)
V_{cy} = V_{cf} (EF)/27
V_{ton} = V_{cy} (2.0)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

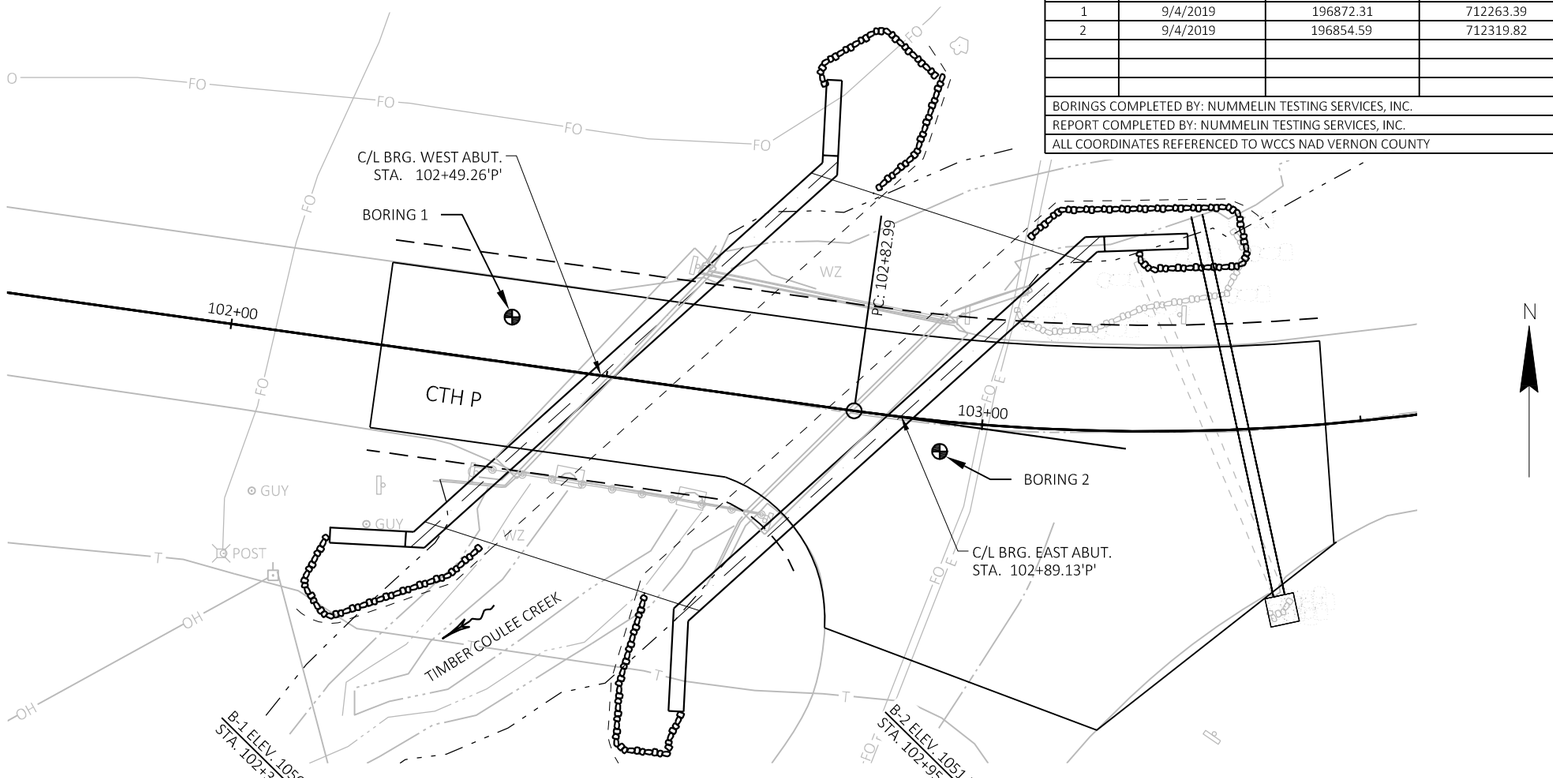
STRUCTURE B-62-260

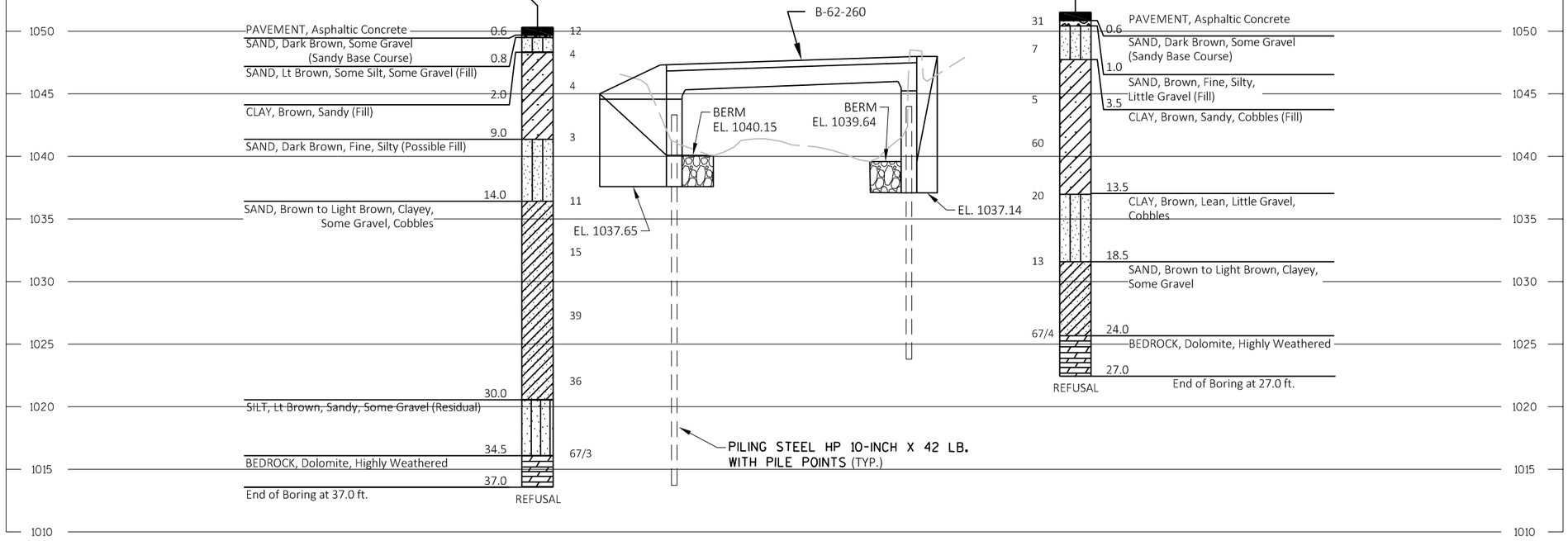
DRAWN BY STD | PLANS CK'D. CAH

CROSS SECTIONS, NOTES & QUANTITIES

SHEET 2 OF 11
35

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	9/4/2019	196872.31	712263.39
2	9/4/2019	196854.59	712319.82
BORINGS COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
REPORT COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
ALL COORDINATES REFERENCED TO WCCS NAD VERNON COUNTY			





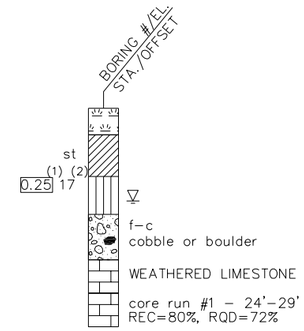
STATE PROJECT NUMBER

5478-00-72

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
boulders or cobbles	LIMESTONE	BEDROCK (unknown)
shale	SANDSTONE	IGNEOUS/ meta

LEGEND OF BORING



(1) Unconfined STRENGTH, as determined by a pocket penetrometer (tsf)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'n' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'n' vaLUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

ground water elevation

- at time of drilling
- end of drilling
- after drilling

ABBREVIATIONS

F-Fine M-Medium C-Coarse st-shelby tube

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDER'S INFORMATION

Borings were completed at points approximately as indicated on this drawing to obtain information concerning the character of subsurface materials found at the site. Because the investigated depths are limited and the area of the borings is very small in relation to the entire site, the Wisconsin Department of Transportation does not warrant similar subsurface conditions below, between, or beyond these borings. Variations in soil conditions should be expected and fluctuations in groundwater levels may occur.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-260

DRAWN BY STD | PLANS CK'D. CAH

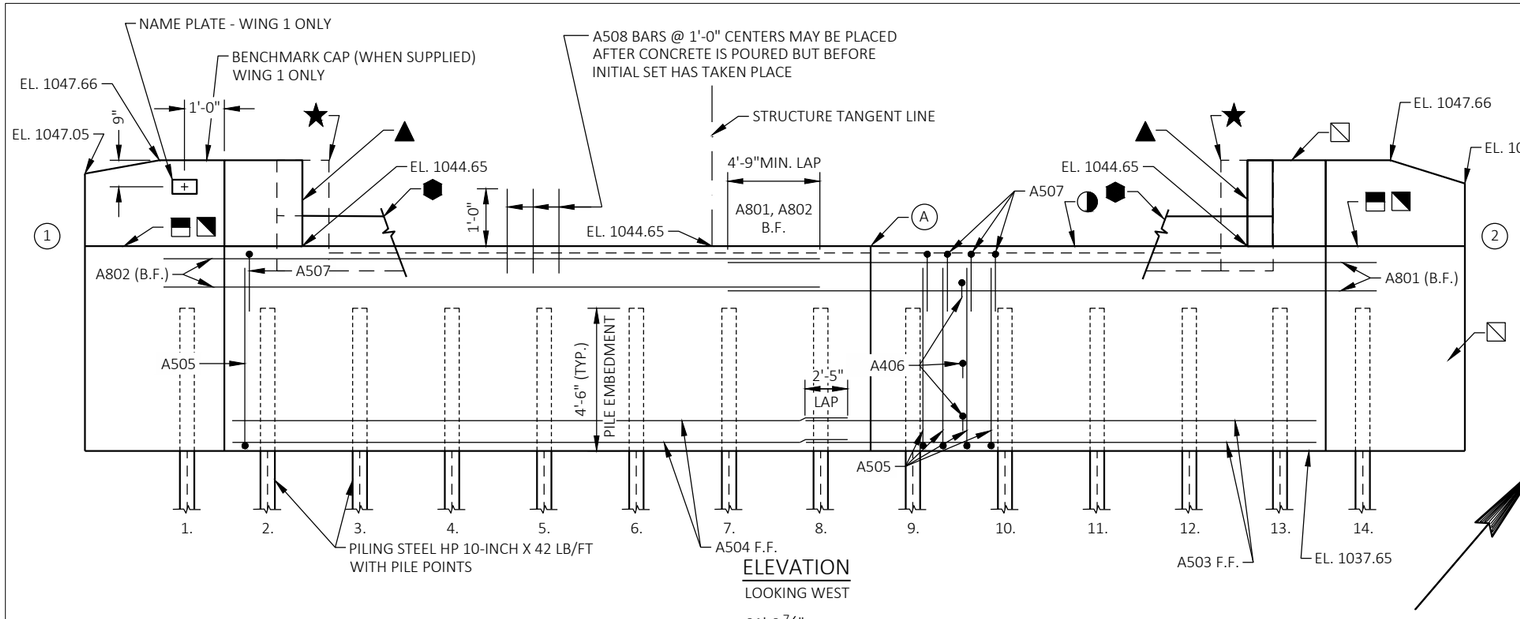
SUBSURFACE EXPLORATION

SHEET 3 OF 11
36

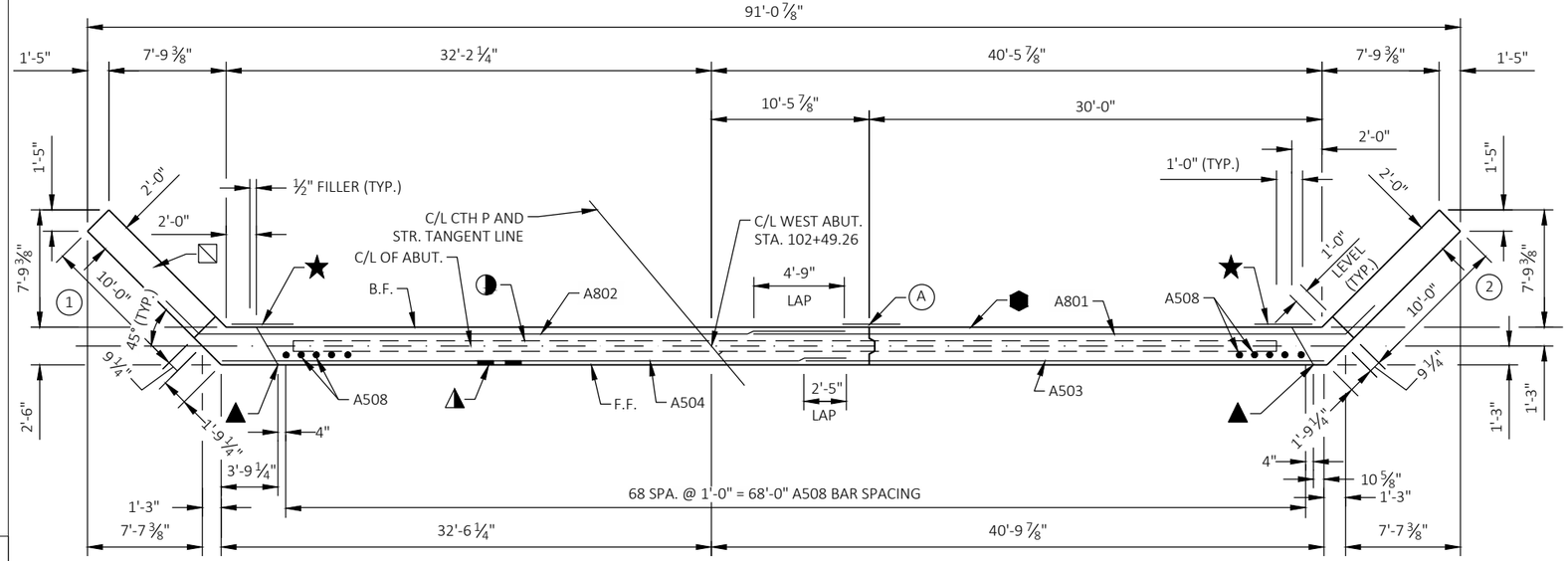
FILE NAME : G:\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\STRUCTURE CAD\080103-SE.DWG

PLOT DATE : 7/7/2020 6:51 AM

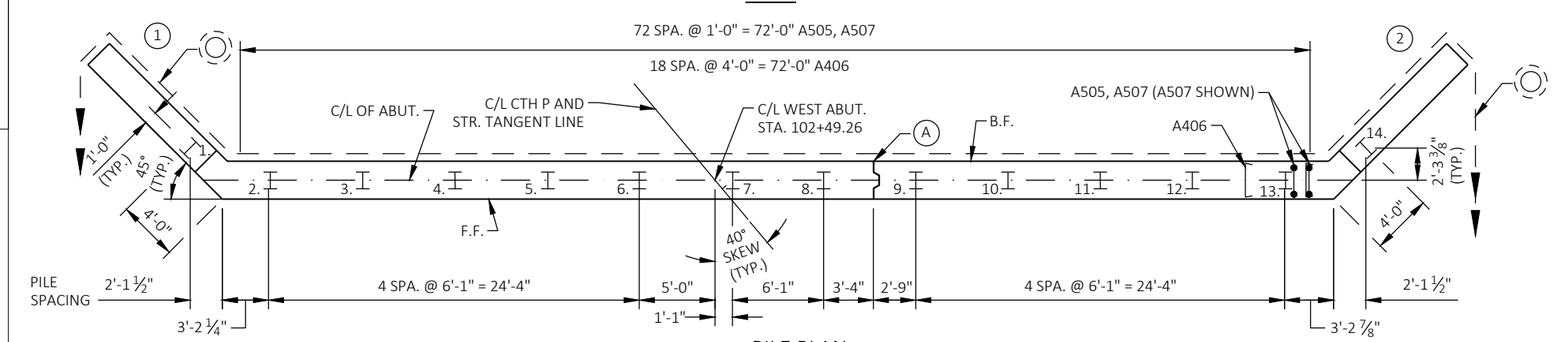
PLOT BY : KL ENGINEERING



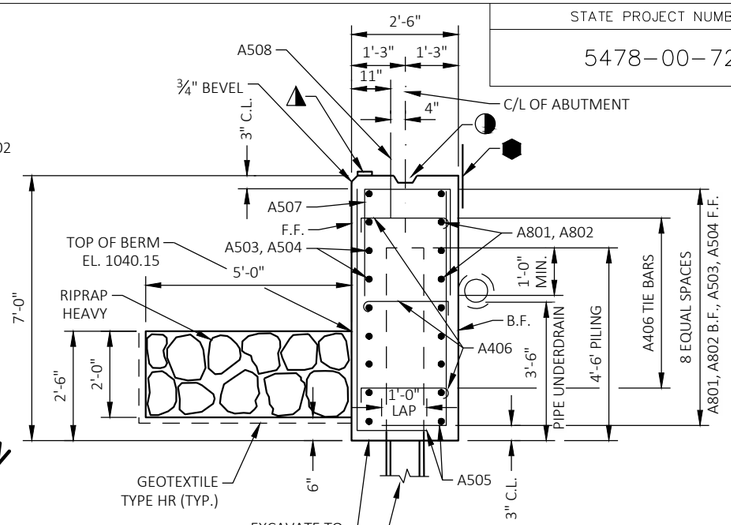
ELEVATION
LOOKING WEST



PLAN



PILE PLAN



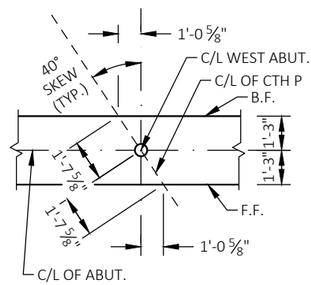
ABUTMENT TO BE SUPPORTED ON PILING STEEL HP 10-INCH X 42 LB/FT WITH PILE POINTS DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 180 TONS PER ABUTMENT BODY AND WING PILE, AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED PILE LENGTHS ARE 30'-0". SEE SHEET 7 FOR PILE SPLICE DETAILS.

TYPICAL SECTION THRU ABUTMENT

LEGEND

- KEYED CONSTRUCTION JOINT FORMED BY BEVELED 2 X 6.
- 1/2" FILLER EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF FILLER WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD 1/8" BELOW SURFACE OF CONCRETE.
- 4" X 3/4" FILLER. EXTEND FULL LENGTH OF ABUTMENT BETWEEN EDGES OF SLAB.
- VERTICAL 18" RUBBERIZED MEMBRANE WATERPROOFING. EXTEND FROM 9" BELOW BRIDGE SEAT TO TOP OF WINGS.
- HORIZONTAL 18" WIDE RUBBERIZED MEMBRANE WATERPROOFING (RMW). EXTEND BETWEEN WINGS.
- OPTIONAL KEYED CONST. JOINT ON WING IS FORMED BY BEVELED 2 X 6. IF JOINT IS USED, PLACE RMW ON B.F. OF WING. COST OF RMW IS INCIDENTAL TO BID ITEM "CONCRETE MASONRY BRIDGES".
- 3/4" "V" GROOVE ON FRONT FACE OF WING WALL. REQUIRED ONLY WHERE CONSTRUCTION JOINT IS USED.
- PIPE UNDERDRAIN WRAPPED 6-INCH, EXTEND THRU GEOTEXTILE FABRIC AT FACE OF RIPRAP HEAVY. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. PROVIDE RODENT SHIELD AT ENDS OF PIPE. FOR RODENT DETAILS, SEE SHEET 5.
- (A) VERT. CONST. JOINT KEYWAY FORMED BY BEVELED 2 X 8. RUN BAR STEEL THRU JOINT. BEVEL FRONT FACE EDGES 3/4" AND SEAL JOINT. PLACE 18" RUBBERIZED MEMBRANE WATERPROOFING ON BACK FACE.
- PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE TOP AND EXTERIOR EXPOSED FACE OF WINGS, AND THE END OF THE FRONT FACE OF ABUTMENT TO 1'-0" BEYOND HEADER.

(X) - INDICATES WING NUMBER F.F. - FRONT FACE B.F. - BACK FACE CL. - CLEAR



LAYOUT DETAIL

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-260

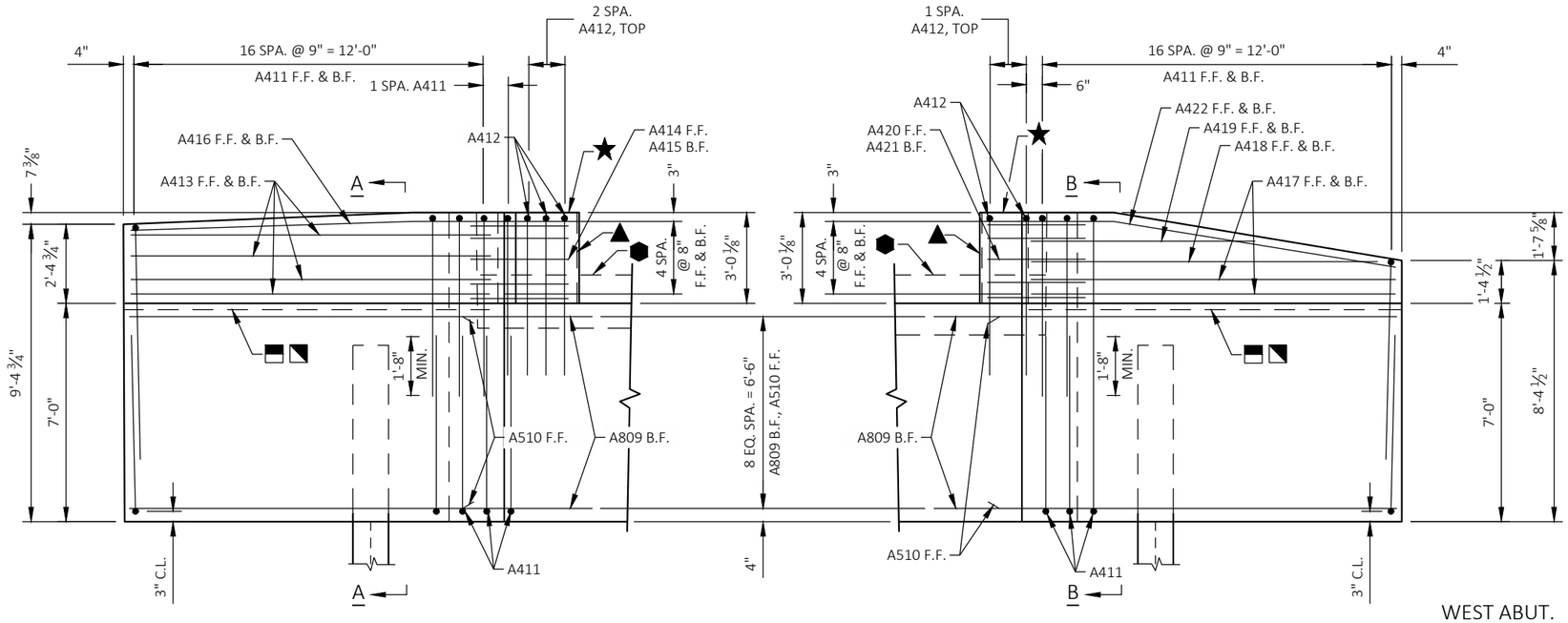
DRAWN BY STD | PLANS CK'D. CAH

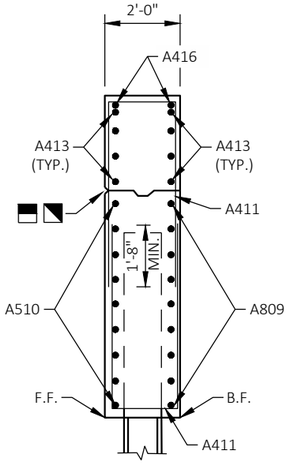
WEST ABUTMENT

SHEET 4 OF 11
37

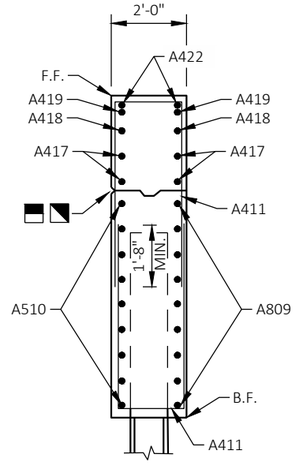
STATE PROJECT NUMBER
5478-00-72

STATE PROJECT NUMBER
5478-00-72



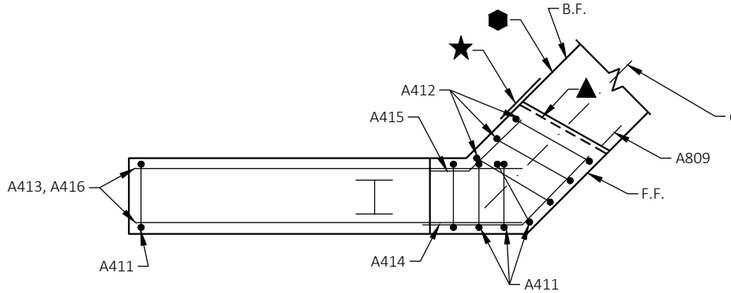


SECTION A-A
THRU WING 1

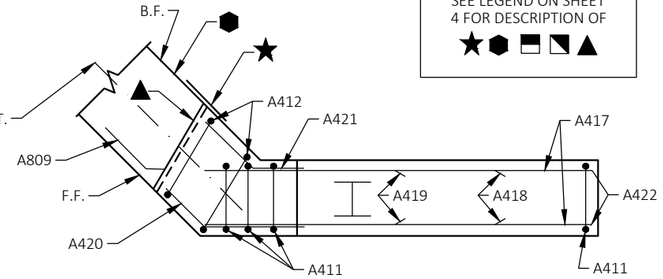


SECTION B-B
THRU WING 2

COATED = 1,980 LBS
UNCOATED = 4,780 LBS



PLAN WING 1



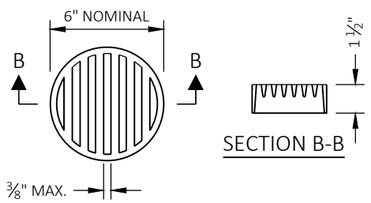
PLAN WING 2

SEE LEGEND ON SHEET 4 FOR DESCRIPTION OF

WEST ABUT.
BILL OF BARS

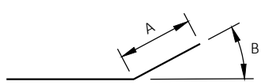
BAR MARK	NO. REQ'D.	LENGTH	COAT	BENT	LOCATION
A801	9	38'-9"		X	ABUT BODY - B.F. - HORIZ.
A802	9	46'-4"		X	ABUT BODY - B.F. - HORIZ.
A503	9	32'-10"			ABUT BODY - F.F. - HORIZ.
A504	9	42'-9"			ABUT BODY - F.F. - HORIZ.
A505	146	8'-1"		X	ABUT BODY - F.F. & B.F. - VERT.
A406	57	3'-0"		X	ABUT BODY - TIES - HORIZ.
A507	73	8'-11"		X	ABUT BODY - TOP - VERT.
A508	69	2'-0"	X		ABUT BODY - DOWELS - VERT.
A809	18	15'-3"	X	X	WINGS - B.F. BOTTOM - HORIZ.
A510	18	13'-8"	X	X	WINGS - F.F. BOTTOM - HORIZ.
A411	70	12'-10"	X	X	WINGS - F.F. & B.F. - TOP & BOTTOM - VERT.
A412	5	12'-0"	X	X	WINGS - F.F. & B.F. - TOP - VERT.
A413	8	12'-2"	X		WING 1 - F.F. & B.F. - HORIZ
A414	5	5'-9"	X	X	WING 1 - F.F. - HORIZ
A415	5	2'-8"	X	X	WING 1 - B.F. - HORIZ
A416	2	11'-2"	X	X	WING 1 - F.F. & B.F. - TOP - HORIZ
A417	4	12'-2"	X		WING 2 - F.F. & B.F. - HORIZ
A418	2	11'-10"	X		WING 2 - F.F. & B.F. - HORIZ
A419	2	7'-9"	X		WING 2 - F.F. & B.F. - HORIZ
A420	5	3'-0"	X	X	WING 2 - F.F. - HORIZ
A421	5	2'-7"	X	X	WING 2 - B.F. - HORIZ
A422	2	11'-5"	X	X	WING 2 - F.F. & B.F. - TOP - HORIZ

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.

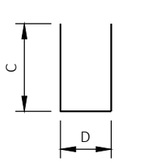


RODENT SHIELD DETAIL

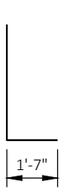
*DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL. THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH". THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.



MARK	A	B
A801	1'-6"	45°
A802	1'-6"	45°
A809	1'-6"	45°
A510	1'-6"	45°
A414	2'-5"	45°
A415	10"	45°
A416	1'-6"	3°
A420	9"	45°
A421	10"	45°
A422	1'-6"	10°



MARK	C	D
A507	3'-6"	2'-2"
A411	5'-8"	1'-8"
A412	4'-10"	2'-6"



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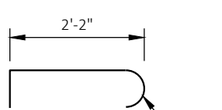

A406

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-260

DRAWN BY STD | PLANS CK'D. CAH

WEST ABUTMENT DETAILS

SHEET 5 OF 11
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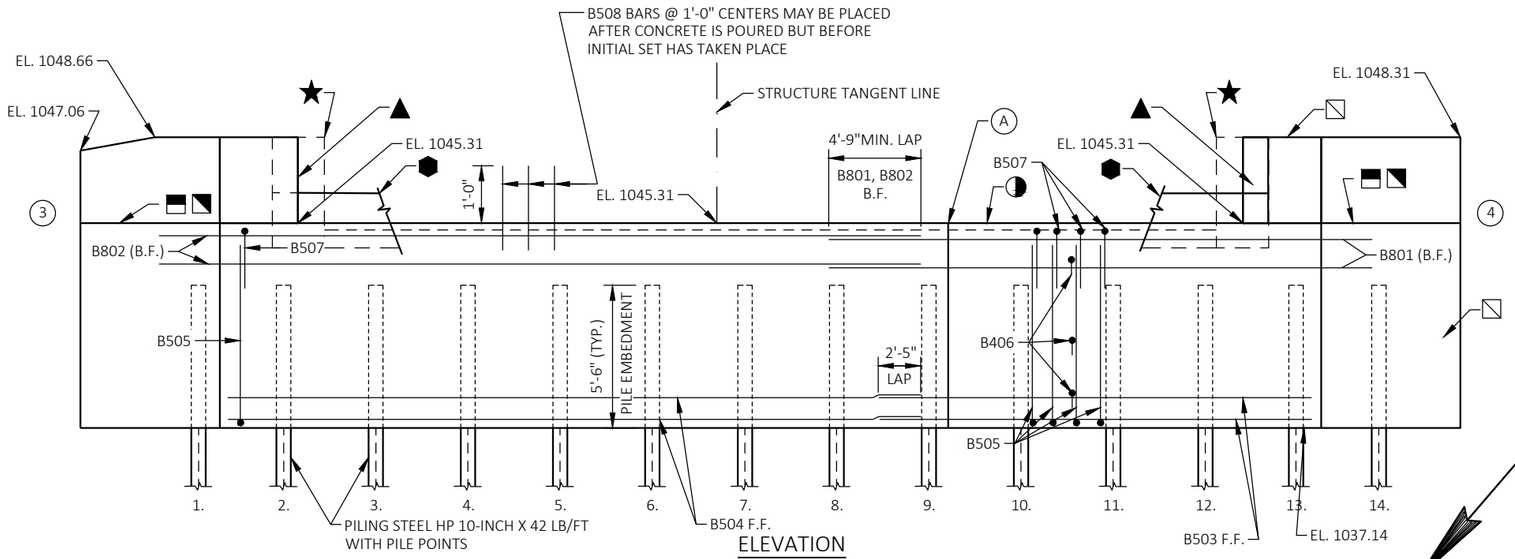
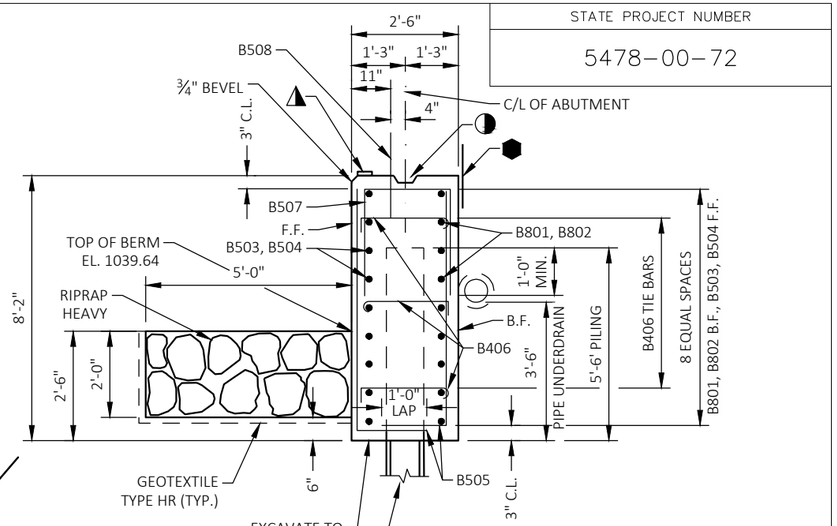


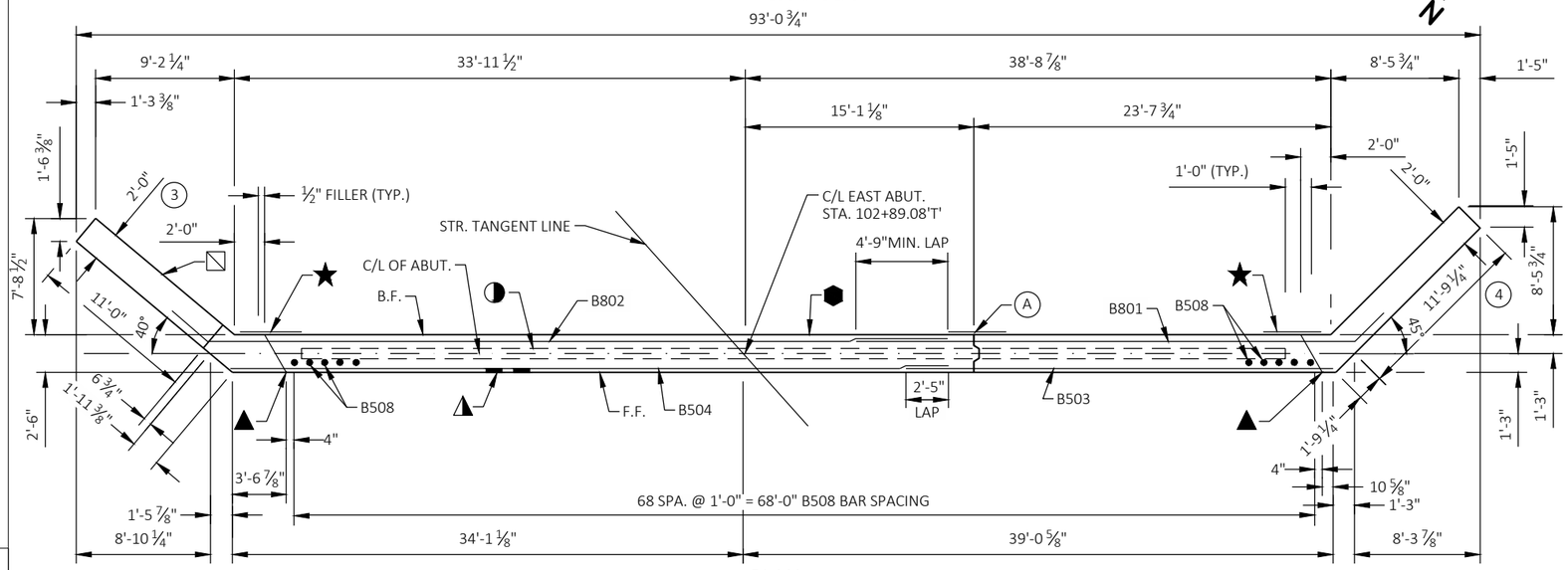
ELEVATION

LOOKING EAST

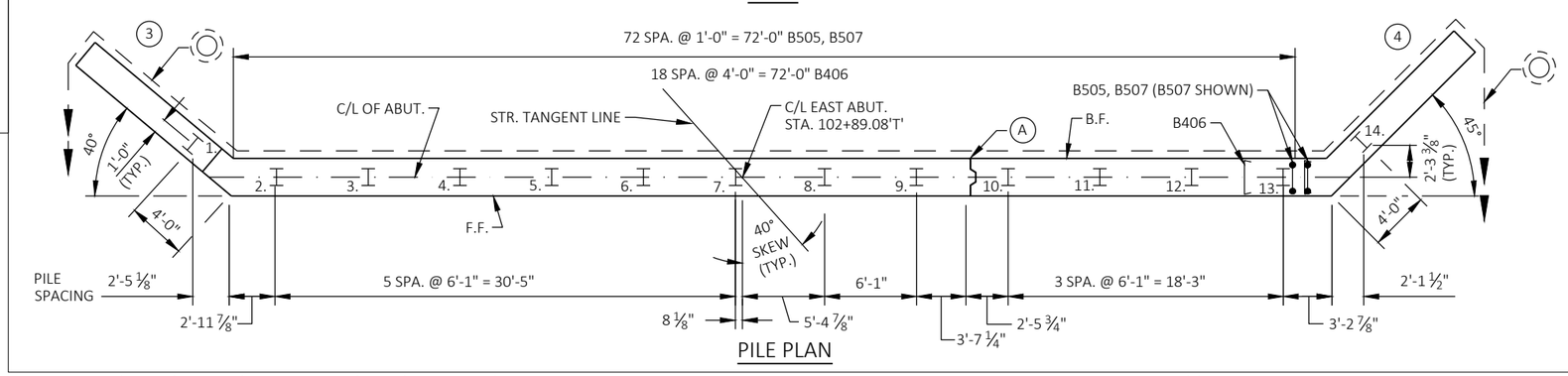


ABUTMENT TO BE SUPPORTED ON PILING STEEL HP 10-INCH X 42 LB/FT WITH PILE POINTS DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 180 TONS PER ABUTMENT BODY AND WING PILE, AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED PILE LENGTHS ARE 20'-0". SEE SHEET 7 FOR PILE SPLICE DETAILS.

TYPICAL SECTION THRU ABUTMENT



PLAN

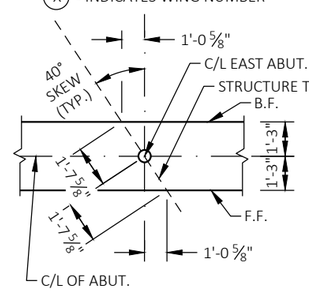


PILE PLAN

LEGEND

- KEYED CONSTRUCTION JOINT FORMED BY BEVELED 2 X 6.
- ½" FILLER EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF FILLER WITH NON-STAINING GRAY, NON-BITUMINOUS JOINT SEALER. (1" DEEP & HOLD ⅛" BELOW SURFACE OF CONCRETE.
- 4" X ¾" FILLER. EXTEND FULL LENGTH OF ABUTMENT BETWEEN EDGES OF SLAB.
- VERTICAL 18" RUBBERIZED MEMBRANE WATERPROOFING. EXTEND FROM 9" BELOW BRIDGE SEAT TO TOP OF WINGS.
- HORIZONTAL 18" WIDE RUBBERIZED MEMBRANE WATERPROOFING (RMW). EXTEND BETWEEN WINGS.
- OPTIONAL KEYED CONST. JOINT ON WING IS FORMED BY BEVELED 2 X 6. IF JOINT IS USED, PLACE RMW ON B.F. OF WING. COST OF RMW IS INCIDENTAL TO BID ITEM "CONCRETE MASONRY BRIDGES".
- ¾" "V" GROOVE ON FRONT FACE OF WING WALL. REQUIRED ONLY WHERE CONSTRUCTION JOINT IS USED.
- PIPE UNDERDRAIN WRAPPED 6-INCH, EXTEND THRU GEOTEXTILE FABRIC AT FACE OF RIPRAP HEAVY. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. PROVIDE RODENT SHIELD AT ENDS OF PIPE. FOR RODENT DETAILS, SEE SHEET 5.
- (A) VERT. CONST. JOINT KEYWAY FORMED BY BEVELED 2 X 8. RUN BAR STEEL THRU JOINT. BEVEL FRONT FACE EDGES ¾" AND SEAL JOINT. PLACE 18" RUBBERIZED MEMBRANE WATERPROOFING ON BACK FACE.
- PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE TOP AND EXTERIOR EXPOSED FACE OF WINGS, AND THE END OF THE FRONT FACE OF ABUTMENT TO 1'-0" BEYOND HEADER.

(X) - INDICATES WING NUMBER F.F. - FRONT FACE B.F. - BACK FACE CL. - CLEAR



LAYOUT DETAIL

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-260

DRAWN BY STD PLANS CK'D. CAH

EAST ABUTMENT

SHEET 6 OF 11

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STATE PROJECT NUMBER

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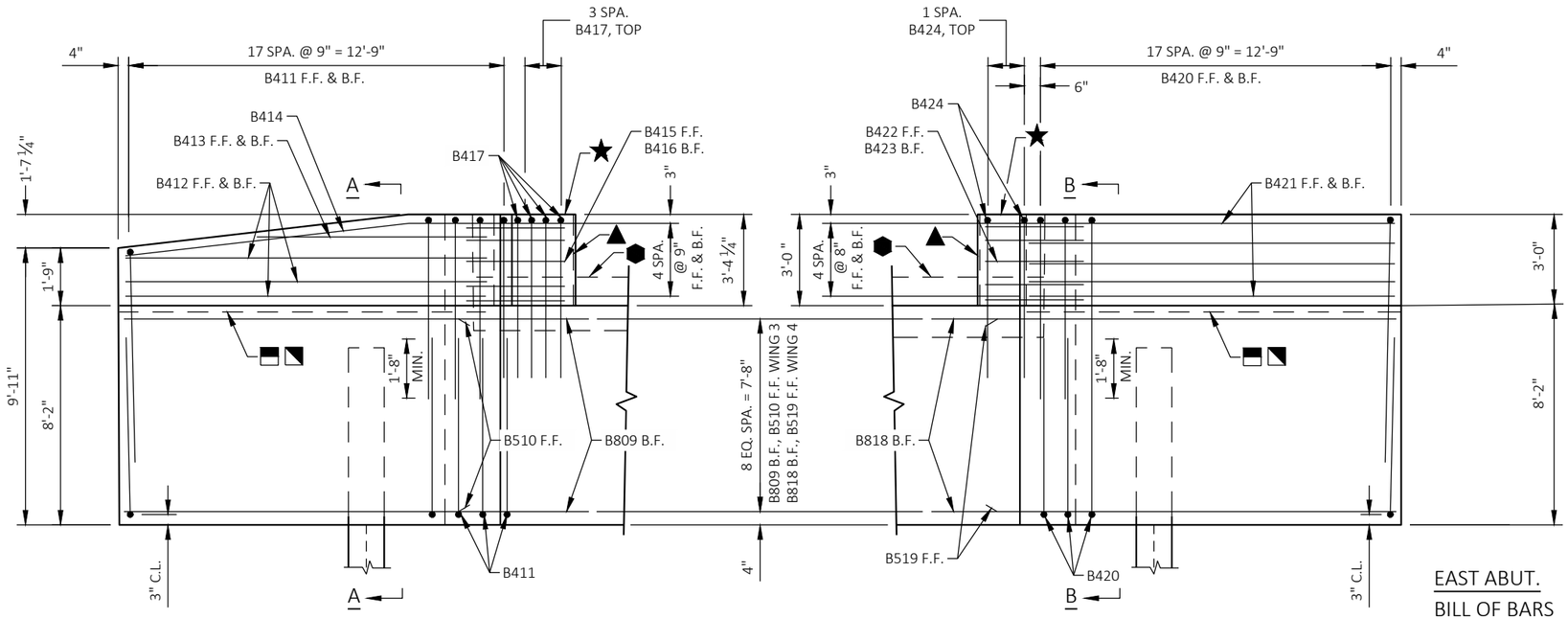
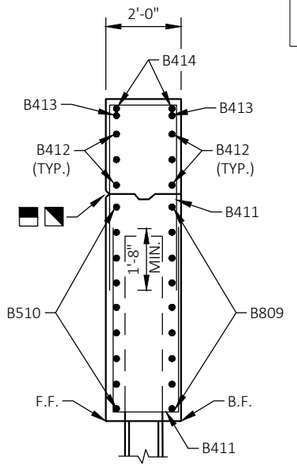
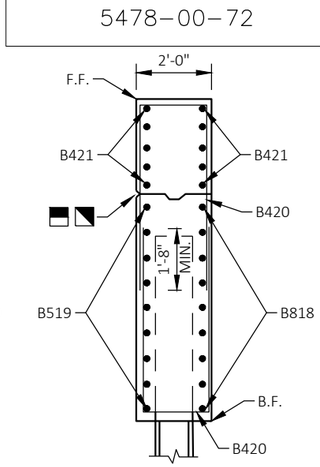
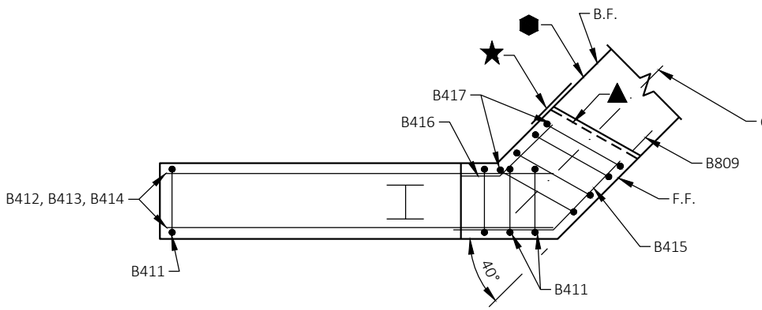




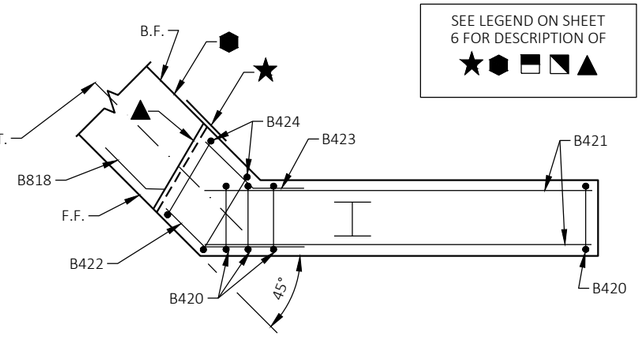
SECTION A-A
THRU WING 3



SECTION B-B
THRU WING 4



PLAN WING 3



PLAN WING 4

SEE LEGEND ON SHEET 6 FOR DESCRIPTION OF

EAST ABUT.

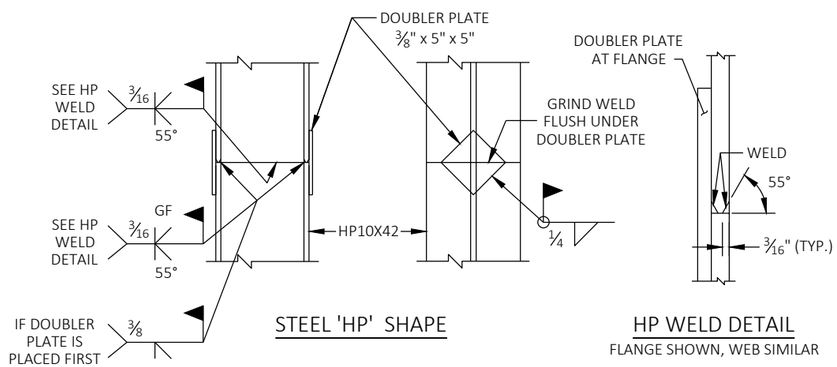
BILL OF BARS

COATED = 2,170 LBS

UNCOATED = 4,980 LBS

BAR MARK	NO. REQ'D.	LENGTH	COAT	BENT	LOCATION
B801	9	32'-5"		X	ABUT BODY - B.F. - HORIZ.
B802	9	52'-11"		X	ABUT BODY - B.F. - HORIZ.
B503	9	26'-5"			ABUT BODY - F.F. - HORIZ.
B504	9	48'-11"			ABUT BODY - F.F. - HORIZ.
B505	146	9'-2"		X	ABUT BODY - F.F. & B.F. - VERT.
B406	57	3'-0"		X	ABUT BODY - TIES - HORIZ.
B507	73	9'-3"		X	ABUT BODY - TOP - VERT.
B508	69	2'-0"	X		ABUT BODY - DOWELS - VERT.
B809	9	16'-7"	X	X	WING 3 - B.F. BOTTOM - HORIZ.
B510	9	14'-8"	X	X	WING 3 - F.F. BOTTOM - HORIZ.
B411	36	14'-4"	X	X	WING 3 - F.F. & B.F. - TOP & BOTTOM - VERT.
B412	6	13'-3"	X		WING 3 - F.F. & B.F. - HORIZ.
B413	2	8'-8"	X		WING 3 - F.F. & B.F. - HORIZ.
B414	2	12'-5"	X	X	WING 3 - F.F. & B.F. - TOP - HORIZ
B415	5	5'-4"	X	X	WING 3 - F.F. - HORIZ
B416	5	2'-8"	X	X	WING 3 - B.F. - HORIZ
B417	4	12'-0"	X	X	WING 3 - F.F. & B.F. - TOP - VERT.
B818	9	16'-2"	X	X	WING 4 - B.F. BOTTOM - HORIZ.
B519	9	14'-8"	X	X	WING 4 - F.F. BOTTOM - HORIZ.
B420	36	14'-2"	X	X	WING 4 - F.F. & B.F. - TOP & BOTTOM - VERT.
B421	10	13'-3"	X		WING 4 - F.F. & B.F. - HORIZ.
B422	5	3'-8"	X	X	WING 4 - F.F. - HORIZ
B423	5	4'-11"	X	X	WING 4 - B.F. - HORIZ
B424	2	11'-4"	X	X	WING 4 - F.F. & B.F. - TOP - VERT.

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.

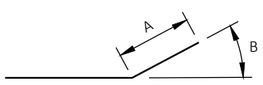


STEEL 'HP' SHAPE

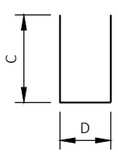
HP WELD DETAIL

FLANGE SHOWN, WEB SIMILAR

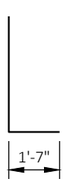
HP 10x42 PILE DETAILS



MARK	A	B
B801	1'-6"	45°
B802	1'-6"	40°
B809	1'-6"	40°
B510	1'-6"	40°
B414	1'-6"	8°
B415	2'-3"	40°
B416	10"	40°
B818	1'-6"	45°
B519	1'-6"	45°
B422	1'-6"	45°
B423	1'-6"	45°



MARK	C	D
B507	3'-8"	2'-2"
B411	6'-5"	1'-8"
B417	4'-10"	2'-6"
B420	6'-4"	1'-8"
B424	4'-6"	2'-6"



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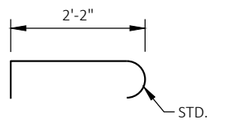

B406

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-260

DRAWN BY STD | PLANS CK'D. CAH

EAST ABUTMENT DETAILS

SHEET 7 OF 11

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STATE PROJECT NUMBER
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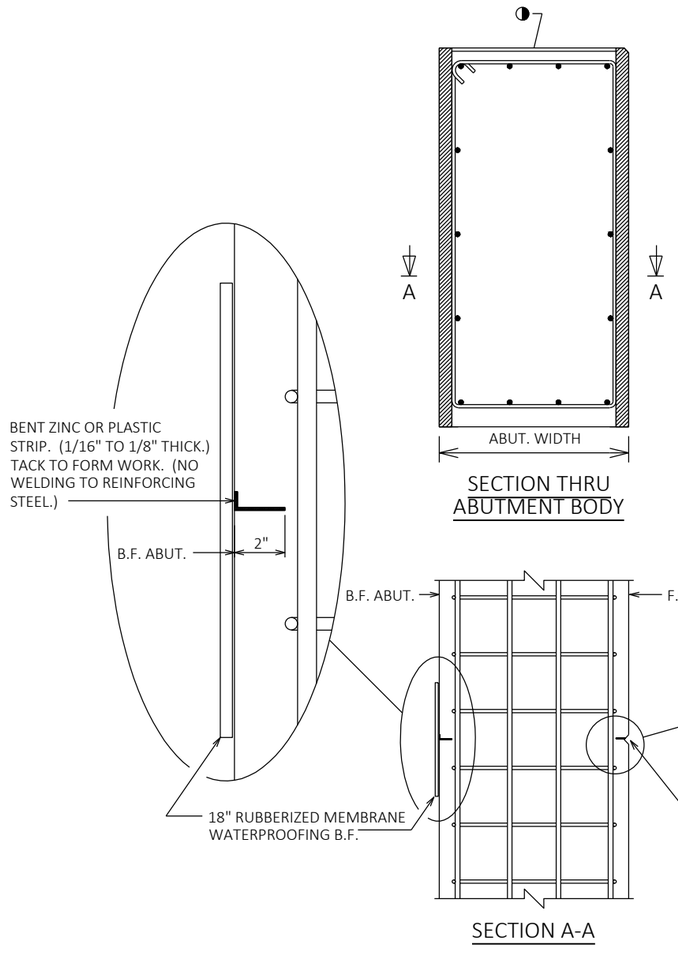

SECTION THRU ABUTMENT BODY

SECTION A-A

ALTERNATE CONSTRUCTION JOINT AT ABUTMENT

NOTES

PARTIAL ZINC OR PLASTIC BULKHEAD MAY BE USED AS ALTERNATE CONSTRUCTION JOINT, WITH THE PERMISSION OF THE ENGINEER, AT THE CONTRACTOR'S EXPENSE.

VERTICAL CONSTRUCTION JOINT KEYWAY IS NOT REQUIRED WHEN USING ALTERNATE CONSTRUCTION JOINT.

CARE IS TO BE USED IN CASTING CONCRETE AROUND BULKHEAD TO PREVENT DISLOCATION OR MISALIGNMENT OF THE BULKHEAD.

SAW CUTTING JOINT IS NOT ALLOWED.

◑ USE A JOINT TOOL TO CONSTRUCT A CONTRACTION JOINT APPROXIMATELY 1/2" DEEP.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-260

DRAWN BY STD | PLANS CK'D. CAH

ALTERNATE CONSTRUCTION JOINT

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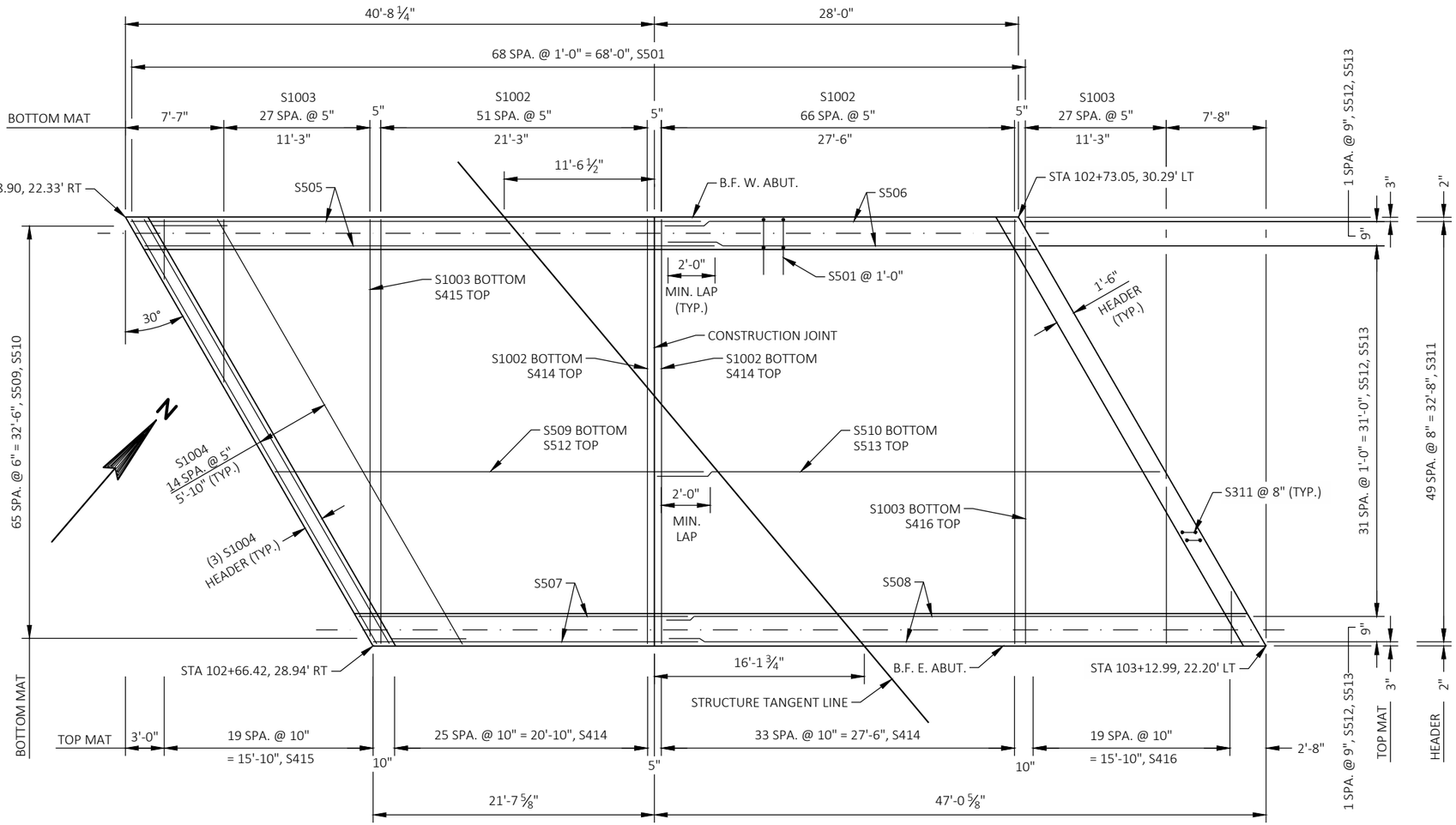
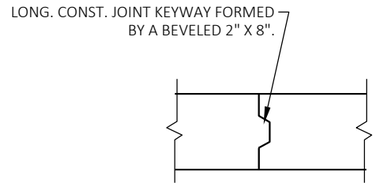
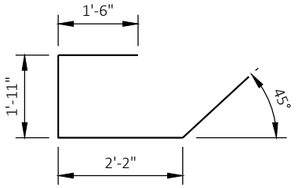


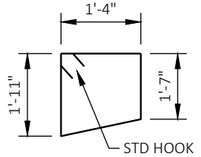
SLAB PLAN



LONGITUDINAL CONSTRUCTION JOINT



S501



S311

BILL OF BARS

TOTAL COATED = 38,600 LBS

BAR MARK	NO. REQ'D.	LENGTH	COAT	STAINLESS STEEL	BENT	BAR SERIES △	LOCATION
S501	138	7'-0"	X		X		DIAPHRAGM @ ABUTS - LONGIT.
S1002	119	32'-8"	X				DECK - LONGIT. - BTM
S1003	56	22'-3"	X			X	DECK - LONGIT. - BTM
S1004	36	37'-8"	X				DECK - LONGIT. - BTM & HEADERS
S505	2	42'-8"	X				DECK - TRANS. @ ABUTS
S506	2	27'-8"	X				DECK - TRANS. @ ABUTS
S507	2	23'-8"	X				DECK - TRANS. @ ABUTS
S508	2	46'-6"	X				DECK - TRANS. @ ABUTS
S509	66	33'-2"	X			X	DECK - TRANS. - BTM
S510	66	37'-1"	X			X	DECK - TRANS. - BTM
S311	100	6'-7"	X		X		DECK - HEADER - VERT.
S512	34	33'-2"	X			X	DECK - TRANS. - TOP
S513	34	37'-1"	X			X	DECK - TRANS. - TOP
S414	60	32'-8"	X				DECK - LONGIT. - TOP
S415	20	18'-4"	X			X	DECK - LONGIT. - TOP
S416	20	17'-10"	X			X	DECK - LONGIT. - TOP

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.

BAR SERIES TABLE

MARK	NO. REQ'D.	LENGTH
S1003	2 SERIES OF 28	12'-6" TO 32'-0"
S509	1 SERIES OF 66	23'-8" TO 42'-8"
S510	1 SERIES OF 66	27'-8" TO 46'-6"
S512	1 SERIES OF 34	23'-8" TO 42'-8"
S513	1 SERIES OF 34	27'-8" TO 46'-6"
S415	1 SERIES OF 20	4'-7" TO 32'-0"
S416	1 SERIES OF 20	4'-1" TO 31'-6"

BUNDLE AND TAG EACH SERIES SEPARATELY.

△ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

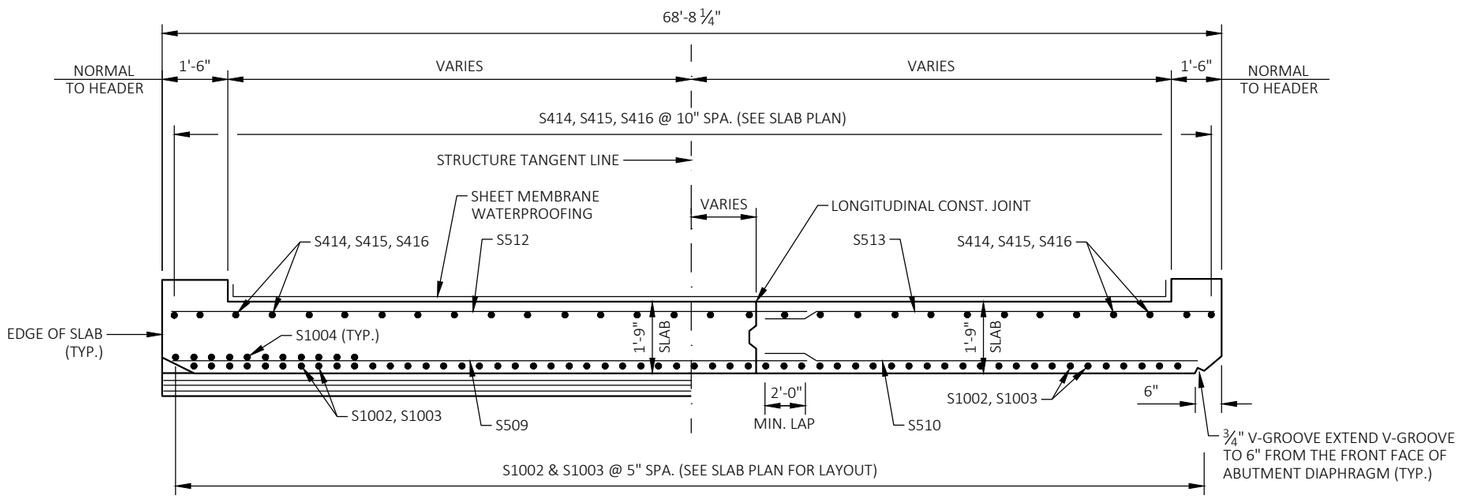
STRUCTURE B-62-260

DRAWN BY STD | PLANS CK'D. CAH

SUPERSTRUCTURE

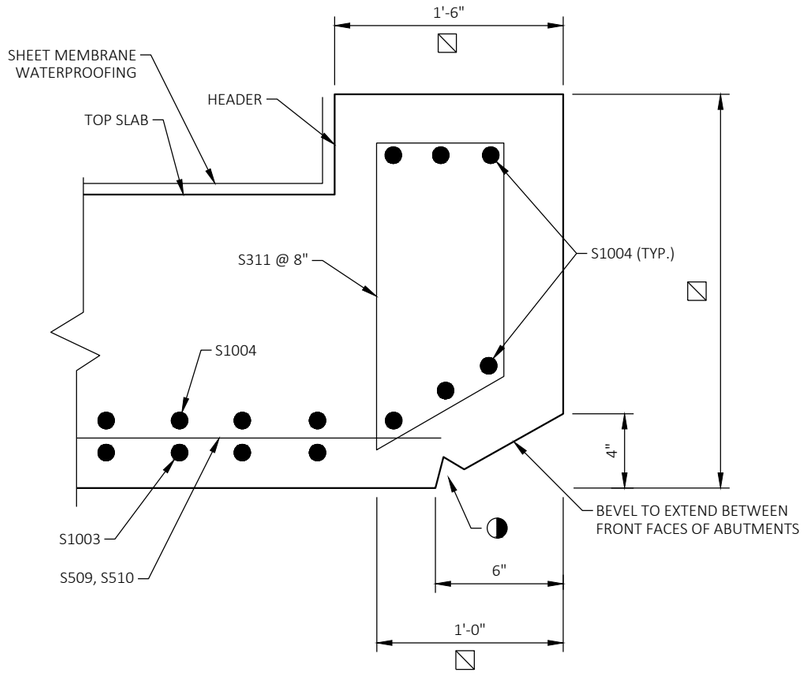
SHEET 9 OF 11
42

STATE PROJECT NUMBER
5478-00-72

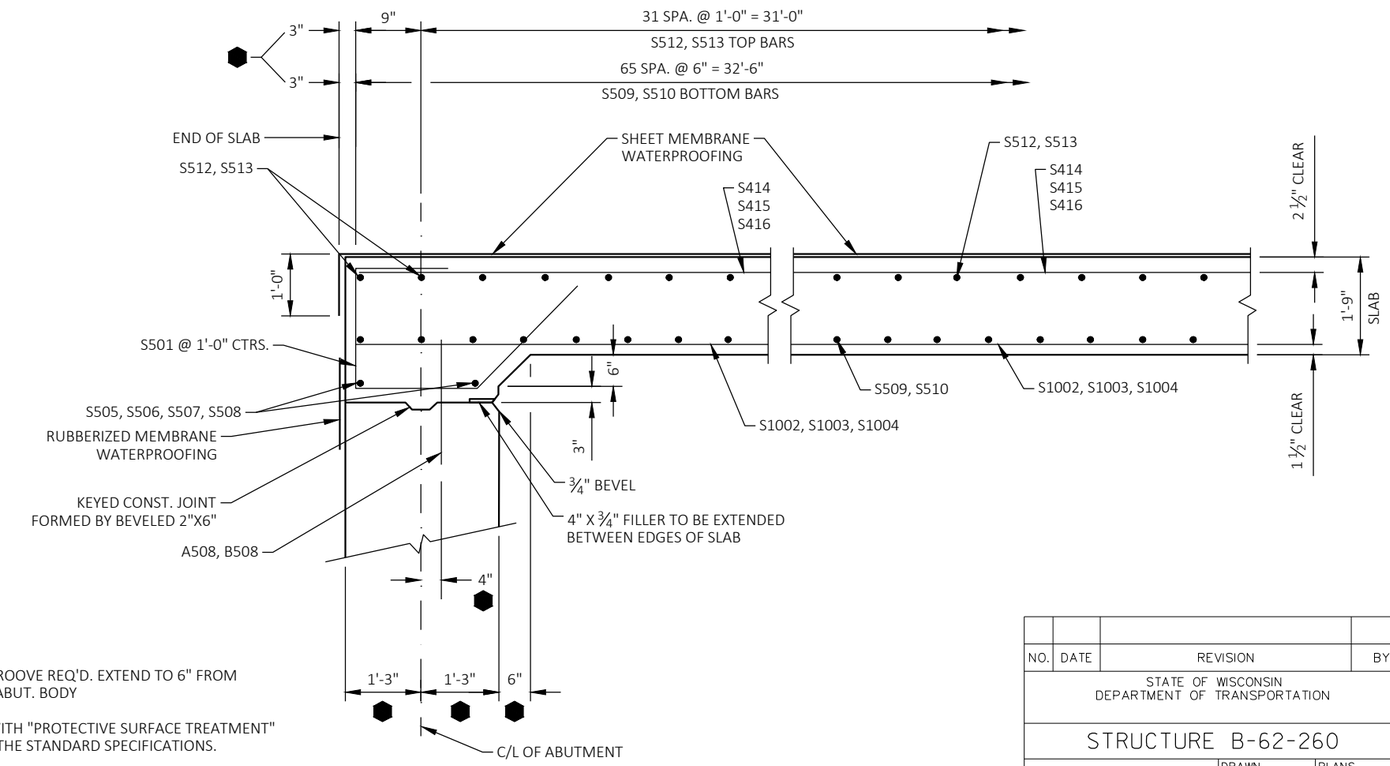


CROSS SECTION THRU BRIDGE

(LOOKING WEST, NORMAL TO ABUTMENTS)



CROSS SECTION THRU HEADER



PART LONGITUDINAL SECTION

NOTES

- 3/4" V-GROOVE REQ'D. EXTEND TO 6" FROM F.F. OF ABUT. BODY
- COAT WITH "PROTECTIVE SURFACE TREATMENT" AS PER THE STANDARD SPECIFICATIONS.
- DIMENSIONS ARE GIVEN NORMAL TO THE C/L OF ABUTMENTS.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

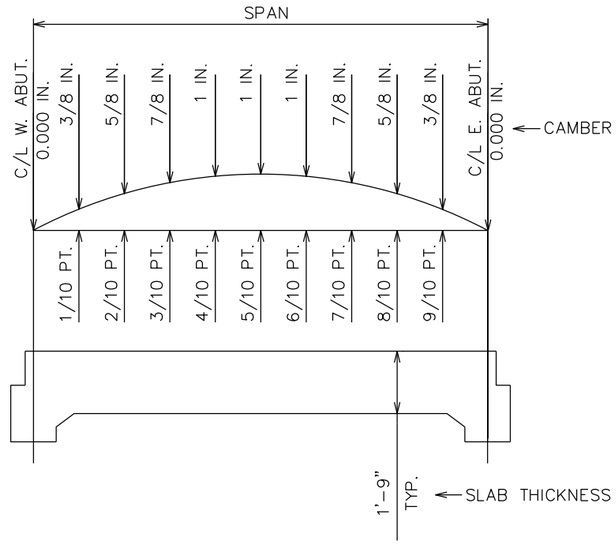
STRUCTURE B-62-260

DRAWN BY STD | PLANS CK'D. CAH

SUPERSTRUCTURE DETAILS 1

SHEET 10 OF 11
43

STATE PROJECT NUMBER
5478-00-72



CAMBER AND SLAB THICKNESS DIAGRAM

CAMBER SHOWN IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. PARAPETS, SIDEWALKS AND MEDIANS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED, EXCEPT FOR STAGED CONSTRUCTION.

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

	TOP OF SLAB ELEVATION AT FINAL GRADE
LESS	SLAB THICKNESS
PLUS	CAMBER
PLUS	FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
EQUALS	TOP OF SLAB FALSEWORK ELEVATION.

SURVEY TOP OF SLAB ELEVATIONS

	W. ABUTMENT	5/10 PT.	E. ABUTMENT
S. HEADER	_____	_____	_____
STRUCTURE TANGENT LINE	_____	_____	_____
N. HEADER	_____	_____	_____

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF DECK ELEVATIONS AT THE C/L OF ABUTMENTS, AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG INSIDE FACE OF HEADERS AND STRUCTURE TANGENT LINE. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

TOP OF SLAB ELEVATIONS

(ELEVATIONS SHOWN ARE FOR EDGES AND CENTERLINE OF SLAB)

℄ BRG. W. ABUT.	1/10	2/10	3/10	4/10	5/10	6/10	7/10	8/10	9/10	℄ BRG. E. ABUT.
1047.15	1047.22	1047.28	1047.35	1047.41	1047.48	1047.55	1047.61	1047.68	1047.74	1047.81

NOTES

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-260

DRAWN BY STD | PLANS CK'D. CAH

SUPERSTRUCTURE DETAILS 2

SHEET 11 OF 11
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CTH P													
		Area				Incremental Volume (Unadjusted)				Cumulative Vol (CY)			
							Salvaged/Unusable				Expanded	Reduced EBS	Mass
		Cut	Salvaged/Unusable	Fill	EBS	Cut	Pavement Material	Fill	EBS	Cut	Fill	In Fill	Ordinate
STATION	Distance		Pavement Material			Note 1	Note 2	Note 3		1.00	1.25	0.80	
		(SF)	(SF)	(SF)	(SF)	(CY)	(CY)	(CY)	(CY)	Note 1		Note 4	Note 5
102+21		43	0	15	2								
102+25	5	41	0	31	2	7	0	4	0	7	4	0	3
102+50	25	41	0	70	2	38	0	47	2	45	61	2	-17
102+75	25	34	0	72	2	35	0	66	2	79	142	3	-63
103+00	25	48	0	56	2	38	0	59	2	117	214	5	-97
103+25	25	48	0	8	2	45	0	29	2	162	249	6	-87
103+37	12	47	0	1	2	21	0	2	1	183	250	7	-67
103+46	9	47	0	0	2	15	0	0	1	198	249	8	-52
						198	0	208	10				

NOTES:

1-CUT — CUT INCLUDES SALVAGED/UNSABLE PAVEMENT MATERIAL

2-SALVAGED/UNSABLE PAVEMENT MATERIAL — THIS DOES NOT SHOW UP IN THE CROSS SECTIONS

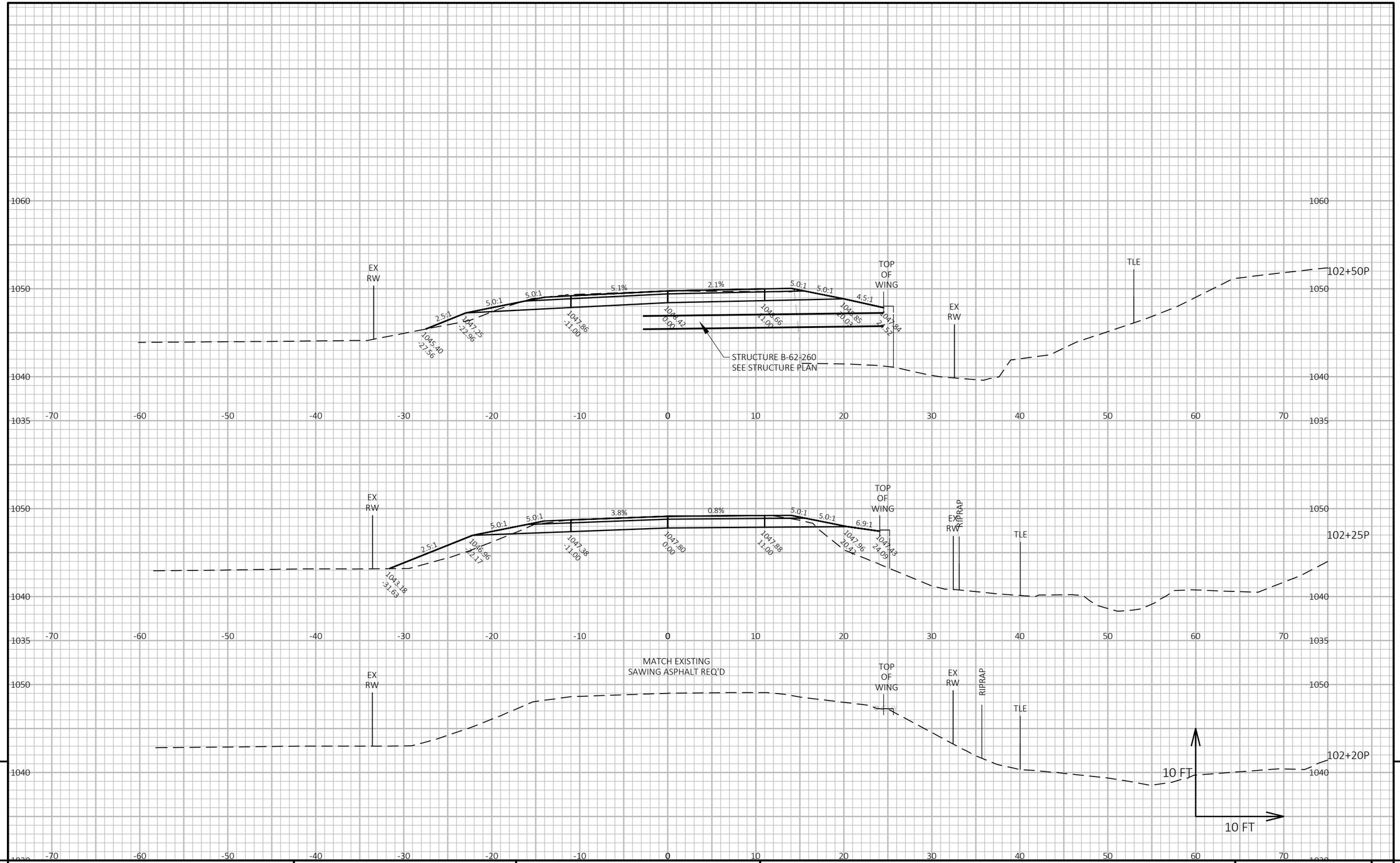
3-FILL — DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME OR SELECT FILL

4-REDUCED EBS IN FILL — REDUCED EBS EXCAVATION THAT CAN BE USED IN FILL

5-MASS ORDINATE — IF EBS TO BE BACKFILLED WITH OR BORRROW: CUT-(FILL * FILL FACTOR AREA UNDER INSIDE 1:1'S EXTENDED DOWN FROM SUBGRADE SHOULDER POINTS
(+) MASS ORDINATE INDICATES WASTE
(-) MASS ORDINATE INDICATES BORROW

9

9

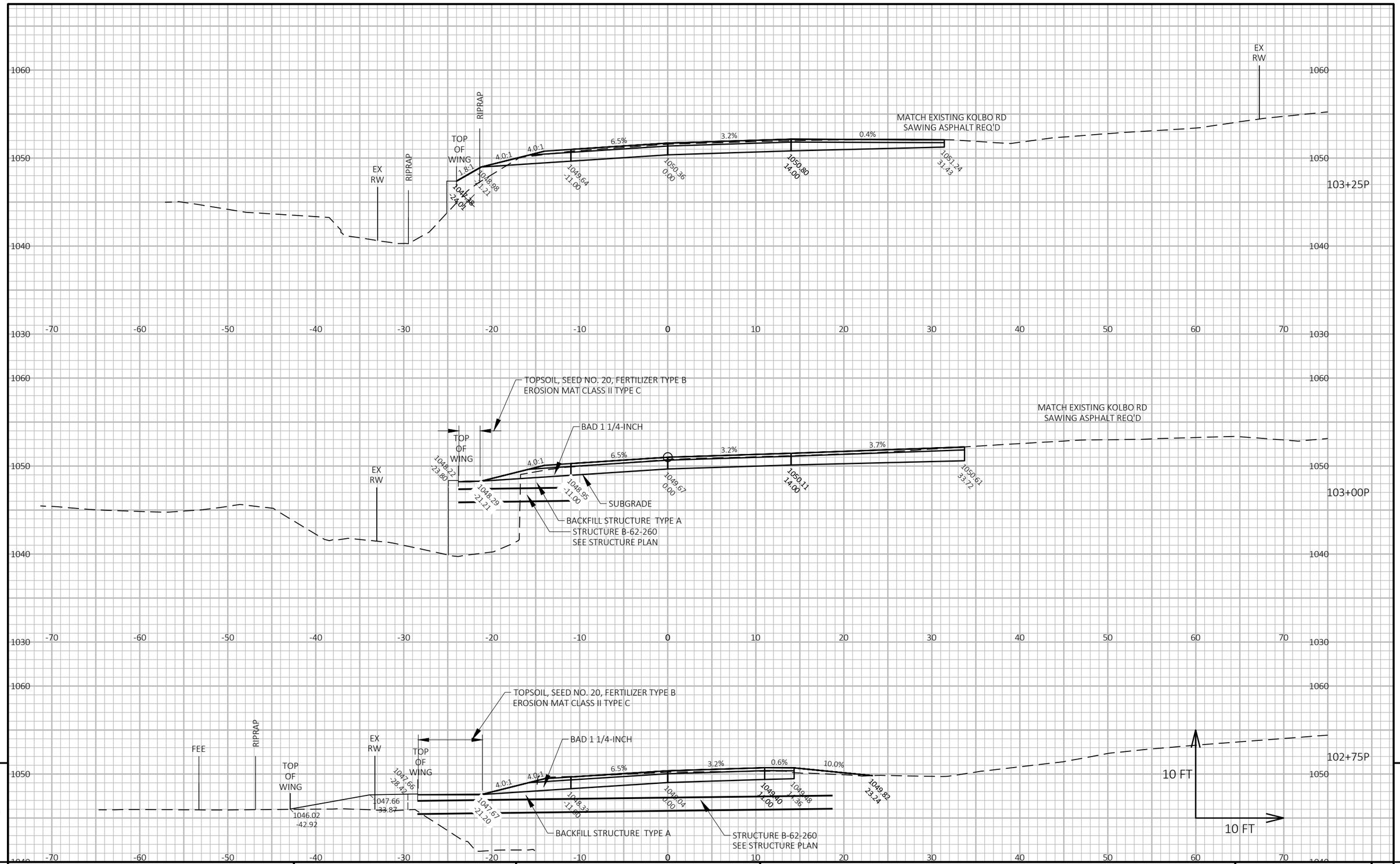


PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	CROSS SECTIONS: CROSS SECTIONS	SHEET 46	E

FILE NAME : \\KLENGINEERING\DATA1\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 1

PLOT DATE : 7/7/2020 10:08 AM PLOT BY : JON BLOMQUIST PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	CROSS SECTIONS: CROSS SECTIONS	SHEET 47	E

FILE NAME : \\KLENGINEERING\DATA1\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\090201-XS-1.DWG
LAYOUT NAME - 2

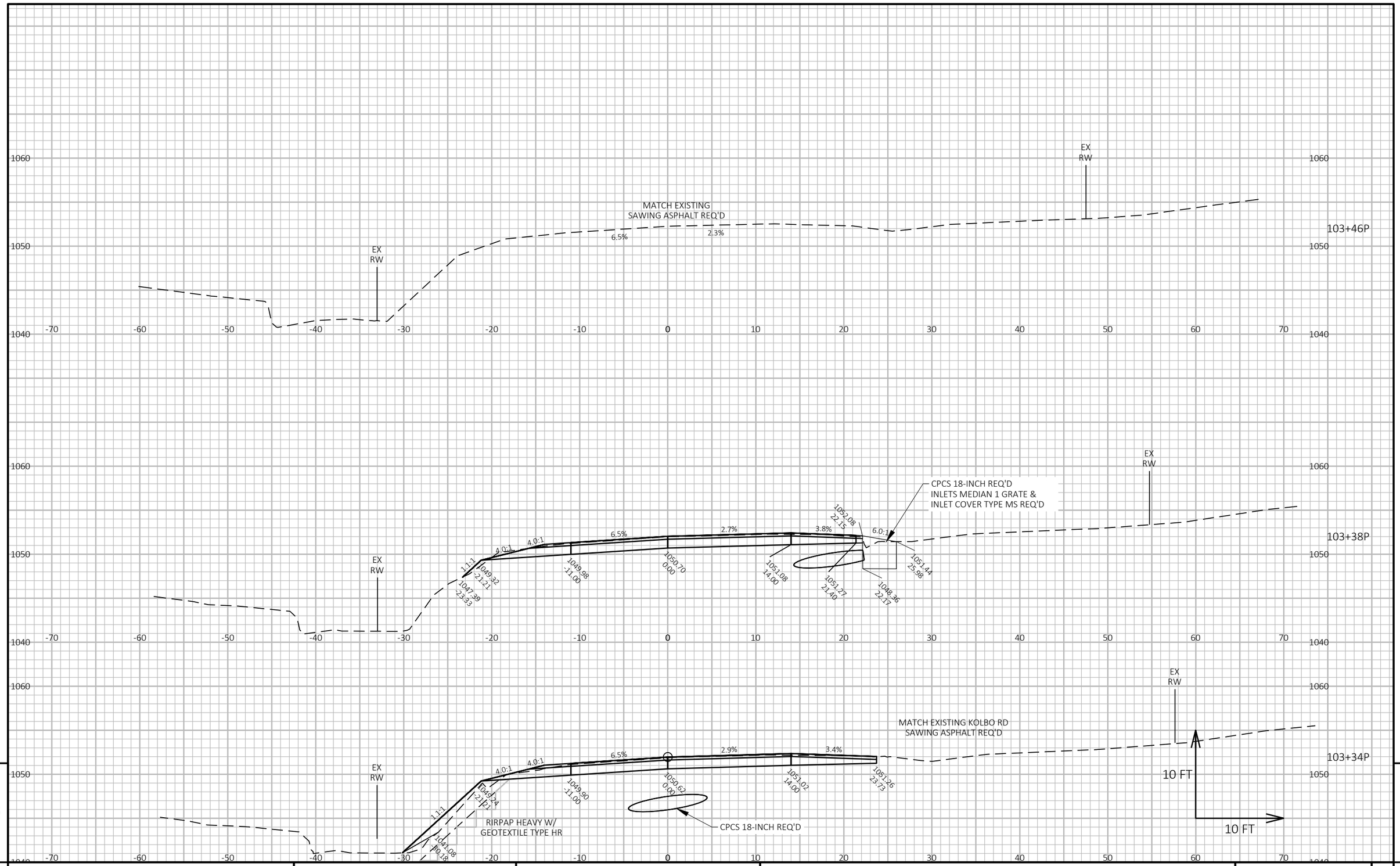
PLOT DATE : 7/8/2020 9:54 AM

PLOT BY : JON BLOMQUIST

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



9

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PROJECT NO: 5478-00-72	HWY: CTH P	COUNTY: VERNON	CROSS SECTIONS: CROSS SECTIONS	SHEET 48	E

FILE NAME : \\KLENGINEERING\DATA1\WDOT LP\VERNON CO\19064_CTH P BRIDGE\CIVIL 3D\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 3

PLOT DATE : 7/7/2020 10:08 AM

PLOT BY : JON BLOMQUIST

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

Notes

Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

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