

| | | |
|-------------|---|------------------------------|
| Section No. | 1 | Title |
| Section No. | 2 | Typical Sections and Details |
| Section No. | 3 | Estimate of Quantities |
| Section No. | 3 | Miscellaneous Quantities |
| Section No. | 4 | Right of Way Plan |
| Section No. | 5 | Plan and Profile |
| Section No. | 6 | Standard Detail Drawings |
| Section No. | 7 | Sign Plates |
| Section No. | 8 | Structure Plans |
| Section No. | 9 | Computer Earthwork Data |
| Section No. | 9 | Cross Sections |

TOTAL SHEETS = 20

PROJECT LOCATION



BEGIN PROJECT
STA 1+57
Y = 479310.87
X = 691415.61

DESIGN DESIGNATION

| | | | |
|--------------|------|---|--------|
| A.A.D.T. | 2021 | = | 100 |
| A.A.D.T. | 2041 | = | 125 |
| D.H.V. | | = | 15 |
| D.D. | | = | 50/50 |
| T. | | = | 3.1% |
| DESIGN SPEED | | = | 35 MPH |
| ESALS | | = | 7,300 |

CONVENTIONAL SYMBOLS

| | |
|--------------------------------|--|
| PLAN | |
| CORPORATE LIMITS | |
| PROPERTY LINE | |
| LOT LINE | |
| LIMITED HIGHWAY EASEMENT | |
| EXISTING RIGHT OF WAY | |
| PROPOSED OR NEW R/W LINE | |
| SLOPE INTERCEPT | |
| REFERENCE LINE | |
| EXISTING CULVERT | |
| PROPOSED CULVERT (Box or Pipe) | |
| COMBUSTIBLE FLUIDS | |
| MARSH AREA | |
| WOODED OR SHRUB AREA | |

| | |
|---|--|
| PROFILE | |
| GRADE LINE | |
| ORIGINAL GROUND | |
| MARSH OR ROCK PROFILE (To be noted as such) | |
| SPECIAL DITCH | |
| GRADE ELEVATION | |
| CULVERT (Profile View) | |
| UTILITIES | |
| ELECTRIC | |
| FIBER OPTIC | |
| GAS | |
| SANITARY SEWER | |
| STORM SEWER | |
| TELEPHONE | |
| WATER | |
| UTILITY PEDESTAL | |
| POWER POLE | |
| TELEPHONE POLE | |

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

T TRIPP, FAIRVIEW ROAD

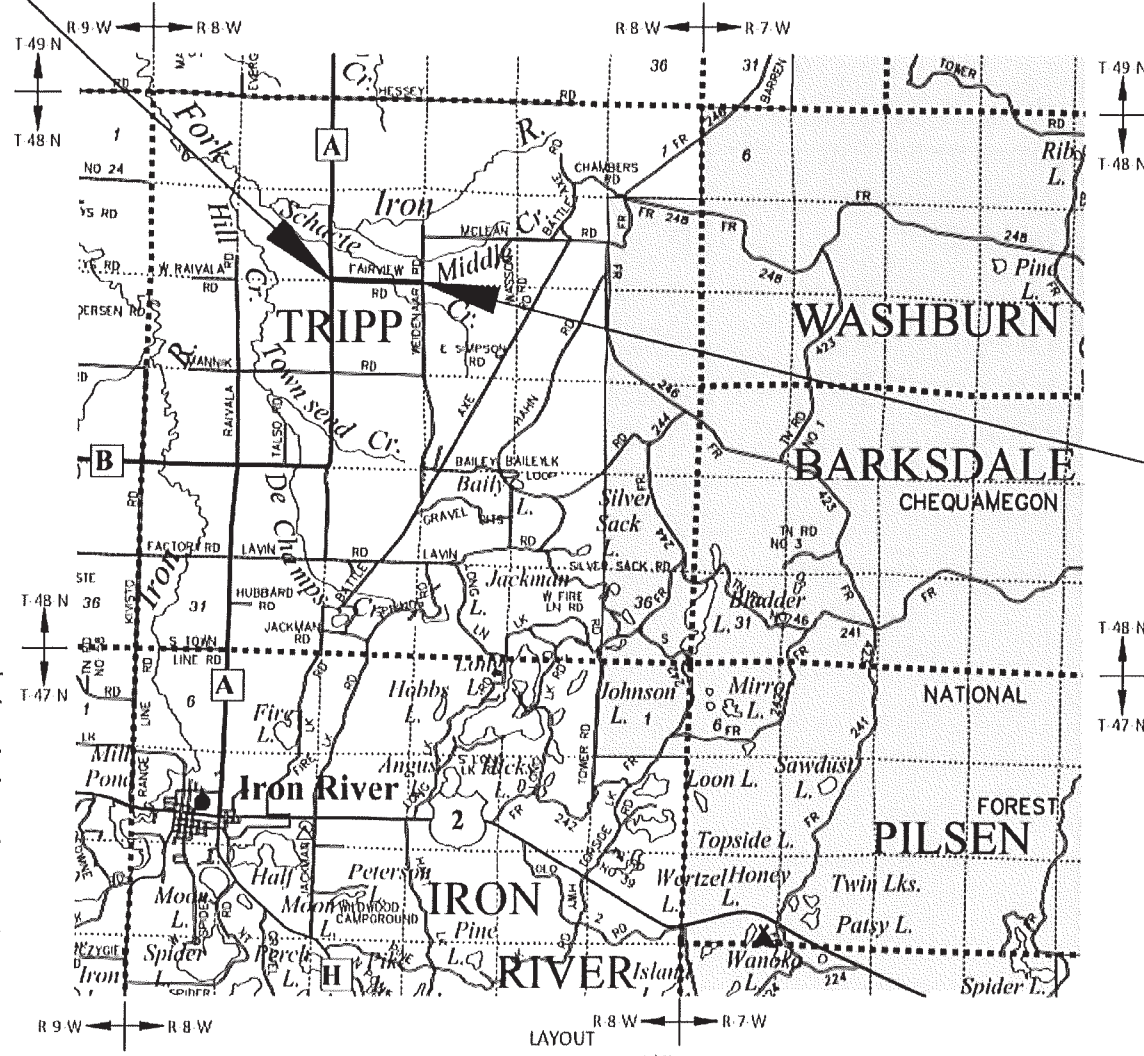
CTH A TO WEIDENAAR ROAD

LOCAL STREET

BAYFIELD COUNTY

STATE PROJECT NUMBER

8358-00-70



END PROJECT
STA 52+44

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), BAYFIELD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ACCEPTED FOR

Town of Tripp

Date 7-25-2020
Town Chairperson Signature

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor: NW REGION
Designer: RALPH MEIR
Project Manager: MATTHEW VAN NATA
Regional Examiner: NW REGION
Regional Supervisor: ANDREW STENSLAND

APPROVED FOR THE DEPARTMENT
DATE: 7/31/2020
Signature

E

UTILITIES

NORVADO
GUY FOLSUM
43705 USH 63
PO BOX 67
CABLE, WI 54821
PHONE: (715)580-8123
EMAIL: gfolsum@norvado.com

BAYFIELD ELECTRIC COOPERATIVE
FRANK POLKOSKI
68460 DISTRICT AVE
PO BOX 68
IRON RIVER, WI 54847
PHONE: (715)372-7540
EMAIL: frank.polkoski@bayfieldelectric.com

GENERAL NOTES:

RESTORE PRIVATE ENTRANCES TO EXISTING CONDITIONS UNLESS SHOWN OTHERWISE.

THE EXACT CONSTRUCTION LIMITS OF PRIVATE ENTRANCES SHALL BE COORDINATED WITH THE ENGINEER IN THE FIELD.

WHEN THE QUANTITY OF BASE AGGREGATE IS MEASURED BY THE TON, THE THICKNESS OF THE LAYER SHOWN ON THE PLAN IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER.

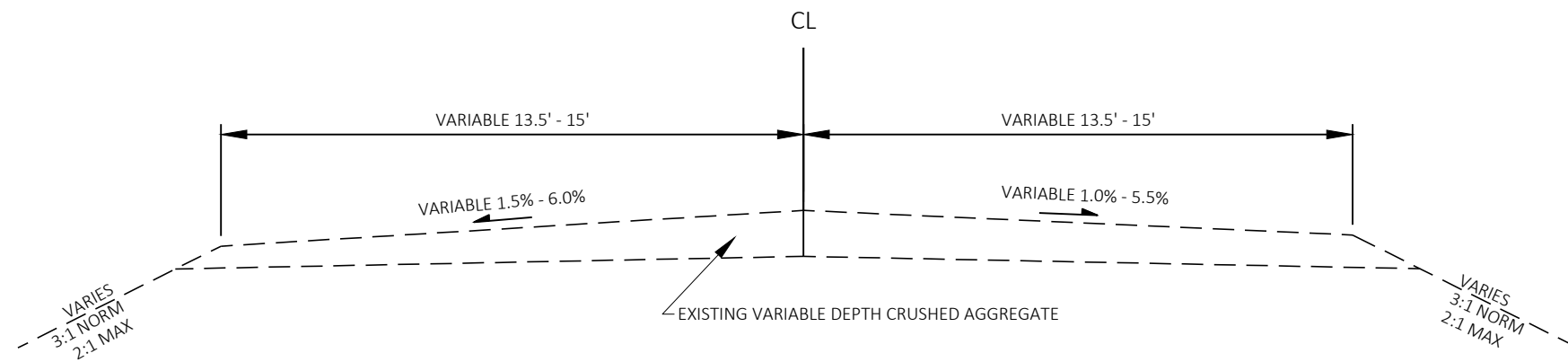
NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY FACILITIES AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.



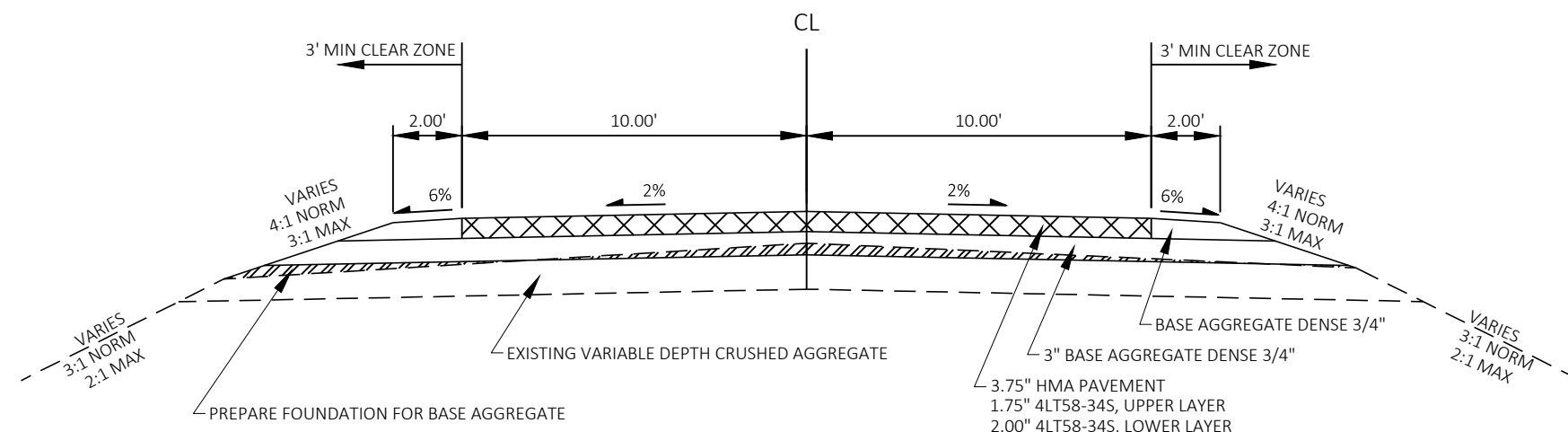
WISCONSIN DEPARTMENT OF NATURAL RESOURCES

WDNR - NORTHWEST DISTRICT HEADQUARTERS
SHAWN HASELEU
810 WEST MAPLE STREET
SPOONER, WI 54801
PHONE: (715)635-4228
EMAIL: Shawn.Haseleu@wisconsin.gov



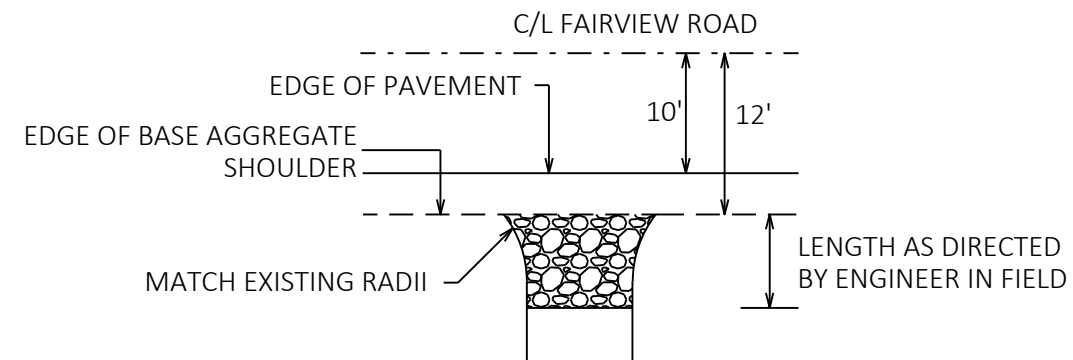
TYPICAL EXISTING SECTION

STA 1+57 TO STA 52+44

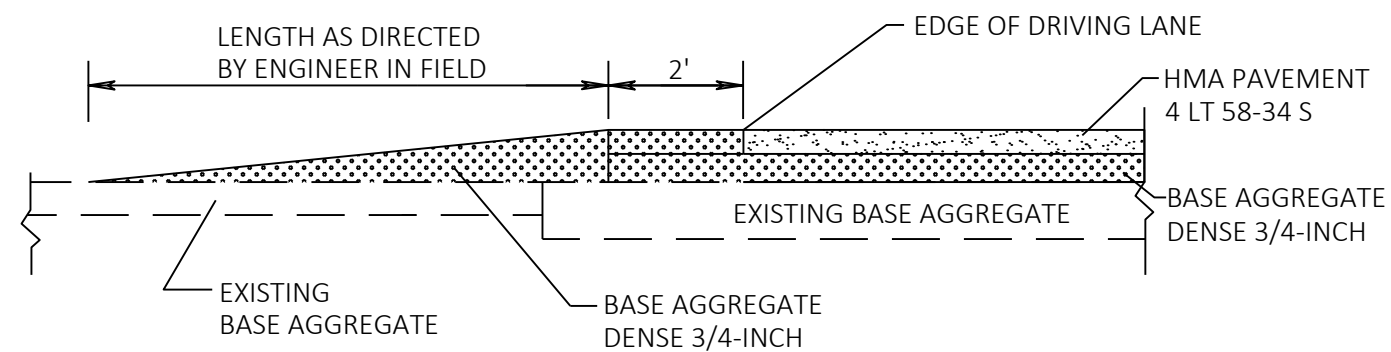


TYPICAL FINISHED SECTION

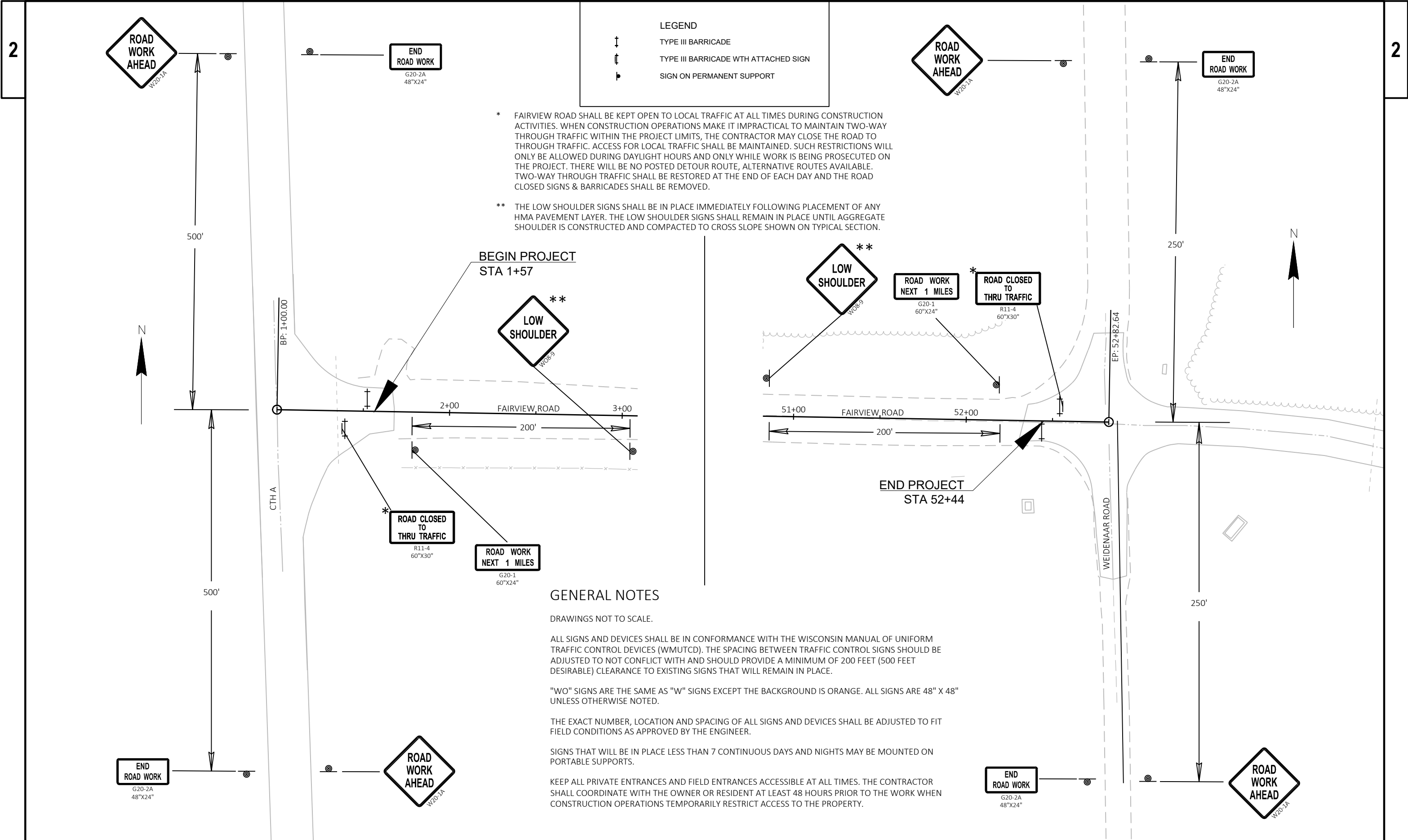
STA 1+57 TO STA 52+44



**BASE AGGREGATE PRIVATE/FIELD ENTRANCE DETAIL
PLAN VIEW**



**BASE AGGREGATE PRIVATE ENTRANCE DETAIL
PROFILE VIEW**



LEGEND

TYPE III BARRICADE

TYPE III BARRICADE WITH ATTACHED SIGN

SIGN ON PERMANENT SUPPORT

* FAIRVIEW ROAD SHALL BE KEPT OPEN TO LOCAL TRAFFIC AT ALL TIMES DURING CONSTRUCTION ACTIVITIES. WHEN CONSTRUCTION OPERATIONS MAKE IT IMPRACTICAL TO MAINTAIN TWO-WAY THROUGH TRAFFIC WITHIN THE PROJECT LIMITS, THE CONTRACTOR MAY CLOSE THE ROAD TO THROUGH TRAFFIC. ACCESS FOR LOCAL TRAFFIC SHALL BE MAINTAINED. SUCH RESTRICTIONS WILL ONLY BE ALLOWED DURING DAYLIGHT HOURS AND ONLY WHILE WORK IS BEING PROSECUTED ON THE PROJECT. THERE WILL BE NO POSTED DETOUR ROUTE, ALTERNATIVE ROUTES AVAILABLE. TWO-WAY THROUGH TRAFFIC SHALL BE RESTORED AT THE END OF EACH DAY AND THE ROAD CLOSED SIGNS & BARRICADES SHALL BE REMOVED.

** THE LOW SHOULDER SIGNS SHALL BE IN PLACE IMMEDIATELY FOLLOWING PLACEMENT OF ANY HMA PAVEMENT LAYER. THE LOW SHOULDER SIGNS SHALL REMAIN IN PLACE UNTIL AGGREGATE SHOULDER IS CONSTRUCTED AND COMPACTED TO CROSS SLOPE SHOWN ON TYPICAL SECTION.

GENERAL NOTES

DRAWINGS NOT TO SCALE.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE. ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

KEEP ALL PRIVATE ENTRANCES AND FIELD ENTRANCES ACCESSIBLE AT ALL TIMES. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER OR RESIDENT AT LEAST 48 HOURS PRIOR TO THE WORK WHEN CONSTRUCTION OPERATIONS TEMPORARILY RESTRICT ACCESS TO THE PROPERTY.

Estimate Of Quantities

| 8358-00-70 | | | | | |
|------------|----------|--|------|-----------|-----------|
| Line | Item | Item Description | Unit | Total | Qty |
| 0002 | 204.0110 | Removing Asphaltic Surface | SY | 56.000 | 56.000 |
| 0004 | 211.0100 | Prepare Foundation for Asphaltic Paving (project) 01. 8358-00-70 | LS | 1.000 | 1.000 |
| 0006 | 211.0500 | Prepare Foundation for Base Aggregate | STA | 50.000 | 50.000 |
| 0008 | 213.0100 | Finishing Roadway (project) 01. 8358-00-70 | EACH | 1.000 | 1.000 |
| 0010 | 305.0110 | Base Aggregate Dense 3/4-Inch | TON | 3,016.000 | 3,016.000 |
| 0012 | 455.0605 | Tack Coat | GAL | 565.000 | 565.000 |
| 0014 | 460.2000 | Incentive Density HMA Pavement | DOL | 1,520.000 | 1,520.000 |
| 0016 | 460.5244 | HMA Pavement 4 LT 58-34 S | TON | 2,374.000 | 2,374.000 |
| 0018 | 618.0100 | Maintenance And Repair of Haul Roads (project) 01. 8358-00-70 | EACH | 1.000 | 1.000 |
| 0020 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0022 | 624.0100 | Water | MGAL | 38.000 | 38.000 |
| 0024 | 643.0420 | Traffic Control Barricades Type III | DAY | 52.000 | 52.000 |
| 0026 | 643.0900 | Traffic Control Signs | DAY | 274.000 | 274.000 |
| 0028 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |
| 0030 | 650.8000 | Construction Staking Resurfacing Reference | LF | 5,087.000 | 5,087.000 |
| 0032 | 650.9910 | Construction Staking Supplemental Control (project) 01. 8358-00-70 | LS | 1.000 | 1.000 |
| 0034 | 690.0150 | Sawing Asphalt | LF | 53.000 | 53.000 |
| 0036 | 740.0440 | Incentive IRI Ride | DOL | 3,850.000 | 3,850.000 |

REMOVING ASPHALTIC SURFACE

| | | | | 204.0110 |
|---------------|----|---------|----------|----------|
| STATION | TO | STATION | LOCATION | SY |
| 1+57 | - | 1+67 | MAINLINE | 31 |
| 52+34 | - | 52+44 | MAINLINE | 25 |
| PROJECT TOTAL | | | | 56 |

BASE AGGREGATE AND WATER

| | | | | 211.0500 | 305.0110 | 624.0100 |
|---------------|----|---------|------------------------|----------|------------|----------|
| STATION | TO | STATION | LOCATION | STA | TON | MGAL |
| 2+00 | - | 52+00 | MAINLINE | 50 | | 14 |
| 1+67 | - | 52+34 | MAINLINE | | 2,406 | 24 |
| 1+57 | - | 52+44 | SHOULDERS DRIVEWAYS | | 500 110 | |
| PROJECT TOTAL | | | | 50 | 3,016 | 38 |

HMA PAVEMENT AND TACK COAT

| | | | | 455.0605 | 460.5244 | |
|---------------|----|---------|----------|------------------|-------------------------------------|-------------|
| STATION | TO | STATION | LOCATION | TACK COAT GAL | HMA PAVEMENT 4 LT 58-34 S TON | REMARKS |
| 1+57 | - | 52+44 | MAINLINE | | 1,266 | LOWER LAYER |
| 1+57 | - | 52+44 | MAINLINE | 565 | 1,108 | UPPER LAYER |
| PROJECT TOTAL | | | | 565 | 2,374 | |

NOTE: ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR ENGINEER
ESTIMATE CATEGORY 0010 UNLESS OTHERWISE NOTED

TRAFFIC CONTROL BARRICADES

| | | | | | 643.0420 |
|----------------|-------------------------------------|-------------------------------|--|----------------------------|----------|
| STATION | NUMBER OF BARRICADES REQUIRED | NUMBER OF DAYS REQUIRED | TRAFFIC CONTROL BARRICADES TYPE 3 DAYS | REMARKS | |
| 1+50 | 1 | 13 | 13 | SEE TRAFFIC CONTROL DETAIL | |
| 1+60 | 1 | 13 | 13 | SEE TRAFFIC CONTROL DETAIL | |
| 52+40 | 1 | 13 | 13 | SEE TRAFFIC CONTROL DETAIL | |
| 52+50 | 1 | 13 | 13 | SEE TRAFFIC CONTROL DETAIL | |
| PROJECT TOTALS | | | 52 | | |

TRAFFIC CONTROL SIGNS

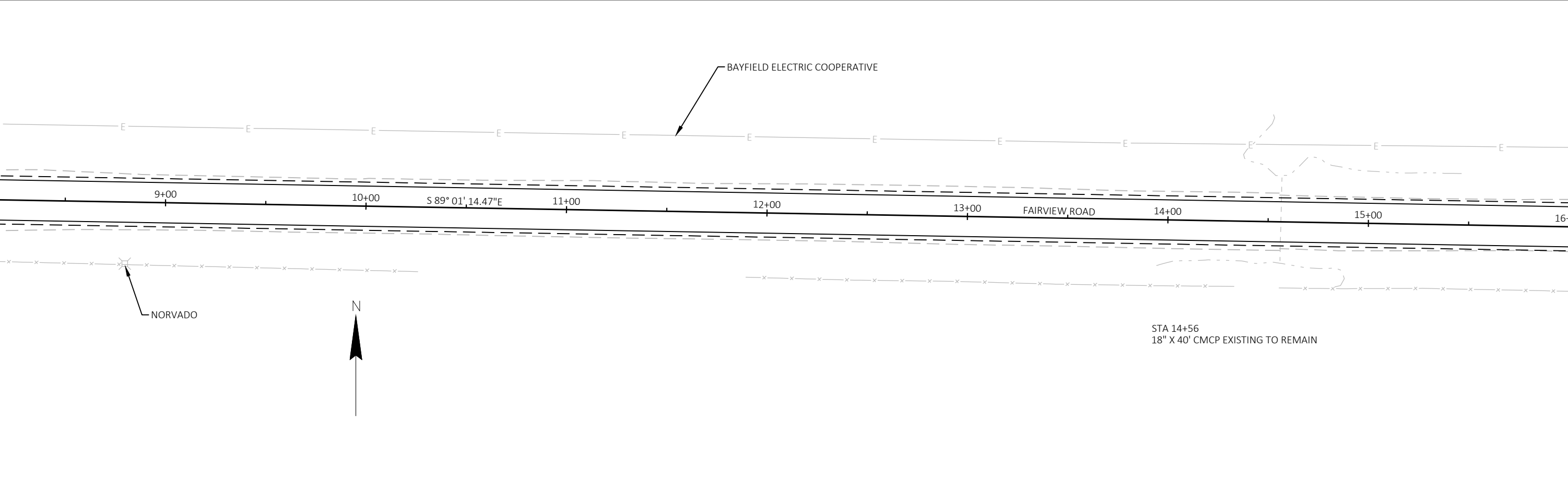
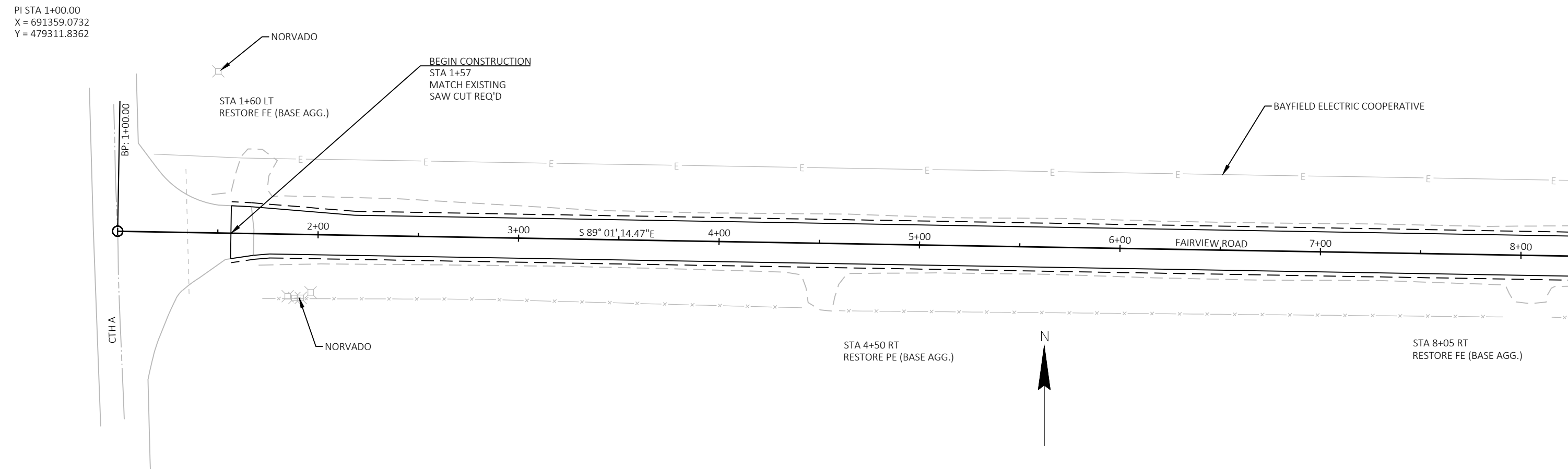
| STATION | LOCATION | SIGN CODE | SIGN MESSAGE | SIZE | 643.0900 DAY | REMARKS |
|-------------------|----------|-----------|-----------------------------|---------|-----------------|----------------------------|
| CTH A NB | RT | W20-1 | ROAD WORK AHEAD | 48 x 48 | 24 | SEE TRAFFIC CONTROL DETAIL |
| CTH A SB | LT | G20-2A | END ROAD WORK | 48 x 24 | 24 | SEE TRAFFIC CONTROL DETAIL |
| CTH A NB | RT | W20-1 | ROAD WORK AHEAD | 48 x 48 | 24 | SEE TRAFFIC CONTROL DETAIL |
| CTH A SB | LT | G20-2A | END ROAD WORK | 48 x 24 | 24 | SEE TRAFFIC CONTROL DETAIL |
| 1+50 | RT | R11-4 | ROAD CLOSED TO THRU TRAFFIC | 60 x 30 | 13 | SEE TRAFFIC CONTROL DETAIL |
| 1+75 | RT | G20-1 | ROAD WORK NEXT 1 MILES | 60 x 24 | 24 | SEE TRAFFIC CONTROL DETAIL |
| 3+75 | RT | W08-9 | LOW SHOULDER | 48 x 48 | 4 | SEE TRAFFIC CONTROL DETAIL |
| 50+25 | LT | W08-9 | LOW SHOULDER | 48 x 48 | 4 | SEE TRAFFIC CONTROL DETAIL |
| 52+25 | LT | G20-1 | ROAD WORK NEXT 1 MILES | 60 x 24 | 24 | SEE TRAFFIC CONTROL DETAIL |
| 52+50 | LT | R11-4 | ROAD CLOSED TO THRU TRAFFIC | 60 x 30 | 13 | SEE TRAFFIC CONTROL DETAIL |
| WEIDENAAR ROAD NB | RT | W20-1 | ROAD WORK AHEAD | 48 x 48 | 24 | SEE TRAFFIC CONTROL DETAIL |
| WEIDENAAR ROAD SB | LT | G20-2A | END ROAD WORK | 48 x 24 | 24 | SEE TRAFFIC CONTROL DETAIL |
| WEIDENAAR ROAD NB | RT | W20-1 | ROAD WORK AHEAD | 48 x 48 | 24 | SEE TRAFFIC CONTROL DETAIL |
| WEIDENAAR ROAD SB | LT | G20-2A | END ROAD WORK | 48 x 24 | 24 | SEE TRAFFIC CONTROL DETAIL |
| PROJECT TOTAL | | | | | 274 | |

CONSTRUCTION STAKING

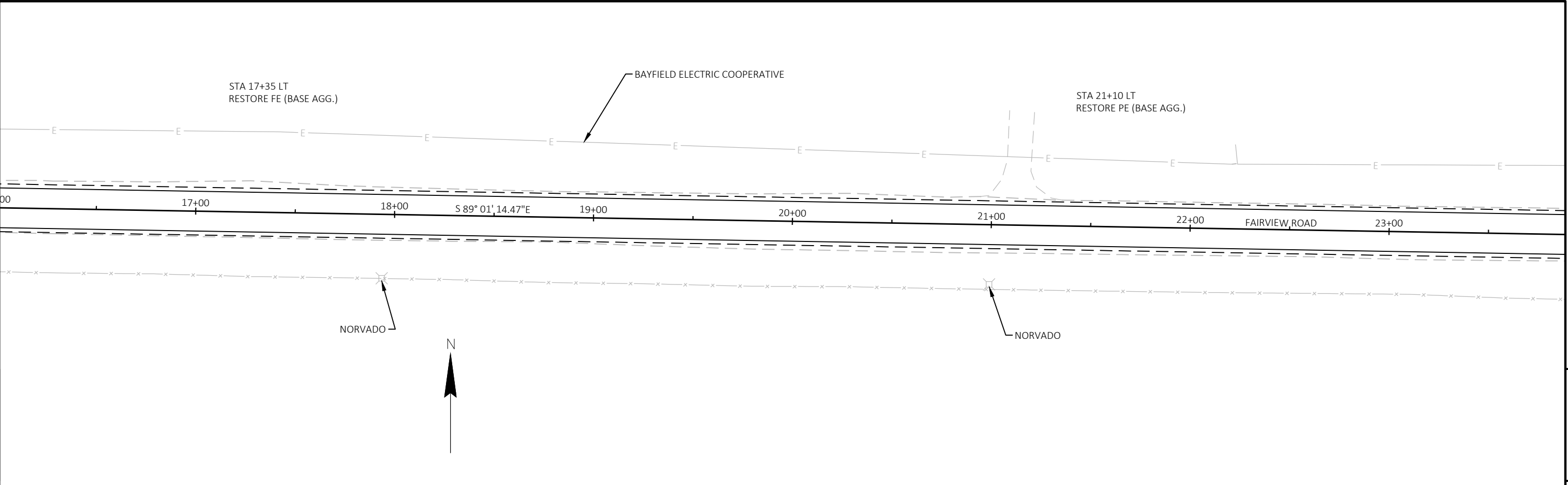
| | | | | 650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF |
|---------------|----|---------|----------|---|
| STATION | TO | STATION | LOCATION | |
| 1+57 | - | 52+44 | MAINLINE | 5,087 |
| PROJECT TOTAL | | | | 5,087 |

SAWING ASPHALT

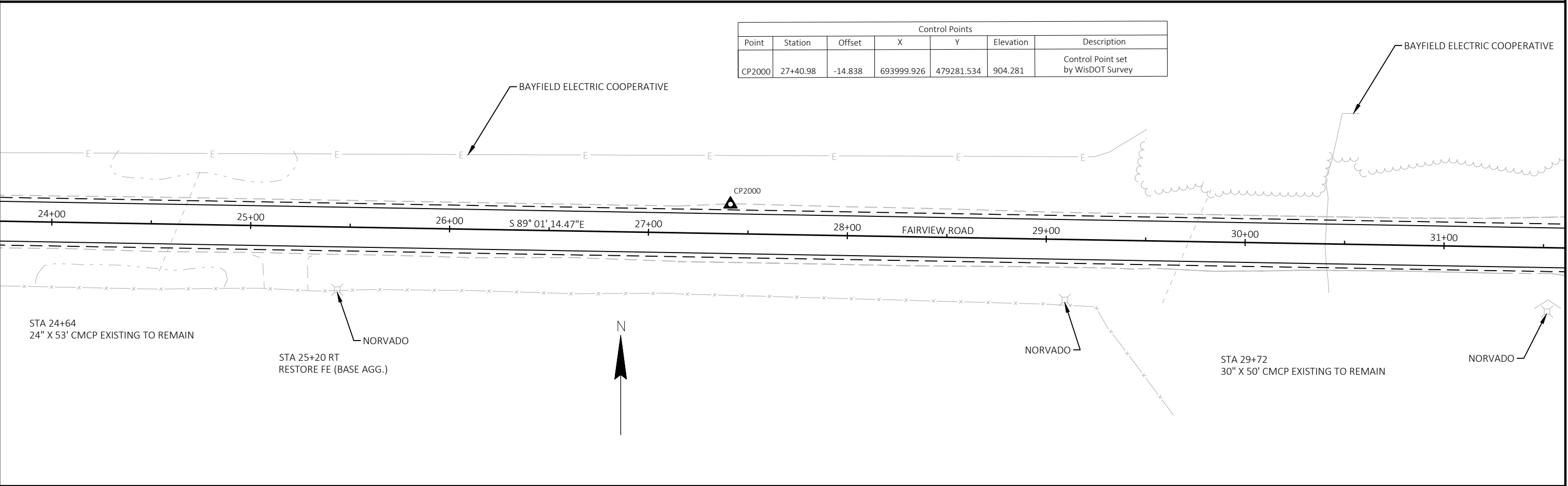
| | | 690.0150 SAWING ASPHALT LF |
|---------------|----------|----------------------------------|
| STATION | LOCATION | |
| 1+57 | MAINLINE | 26 |
| 52+44 | MAINLINE | 27 |
| PROJECT TOTAL | | 53 |



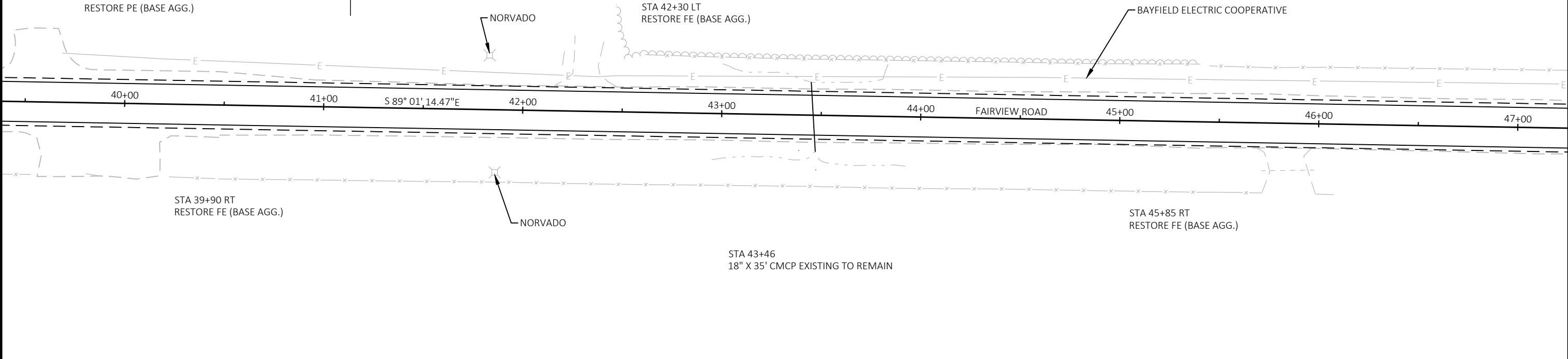
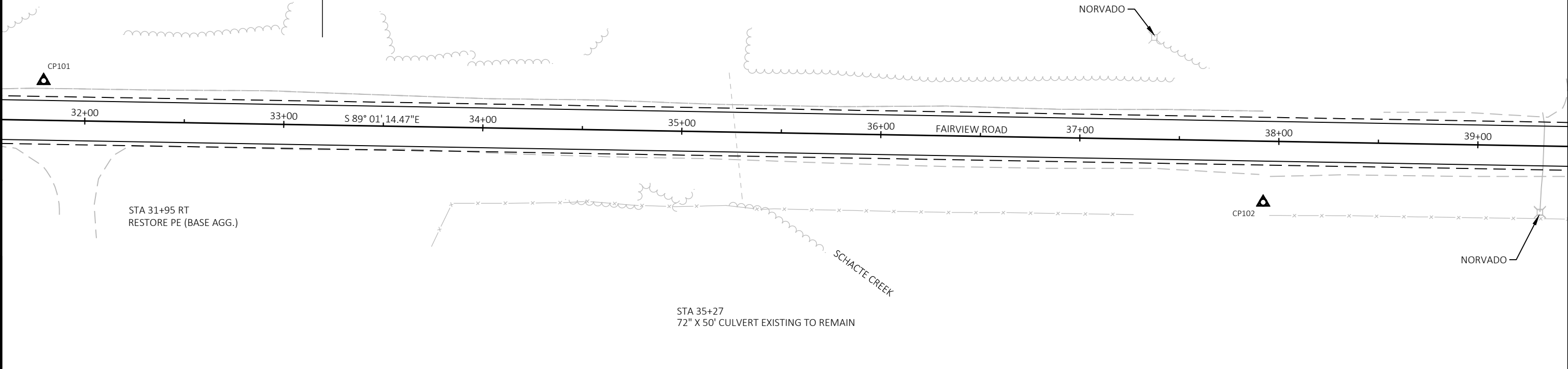
| | | | | | |
|------------------------|--------------------|------------------|-------------|-------|---|
| PROJECT NO: 8358-00-70 | HWY: FAIRVIEW ROAD | COUNTY: BAYFIELD | PLAN SHEETS | SHEET | E |
|------------------------|--------------------|------------------|-------------|-------|---|



| Control Points | | | | | | |
|----------------|----------|---------|------------|------------|-----------|------------------------------------|
| Point | Station | Offset | X | Y | Elevation | Description |
| CP2000 | 27+40.98 | -14.838 | 693999.926 | 479281.534 | 904.281 | Control Point set by WisDOT Survey |

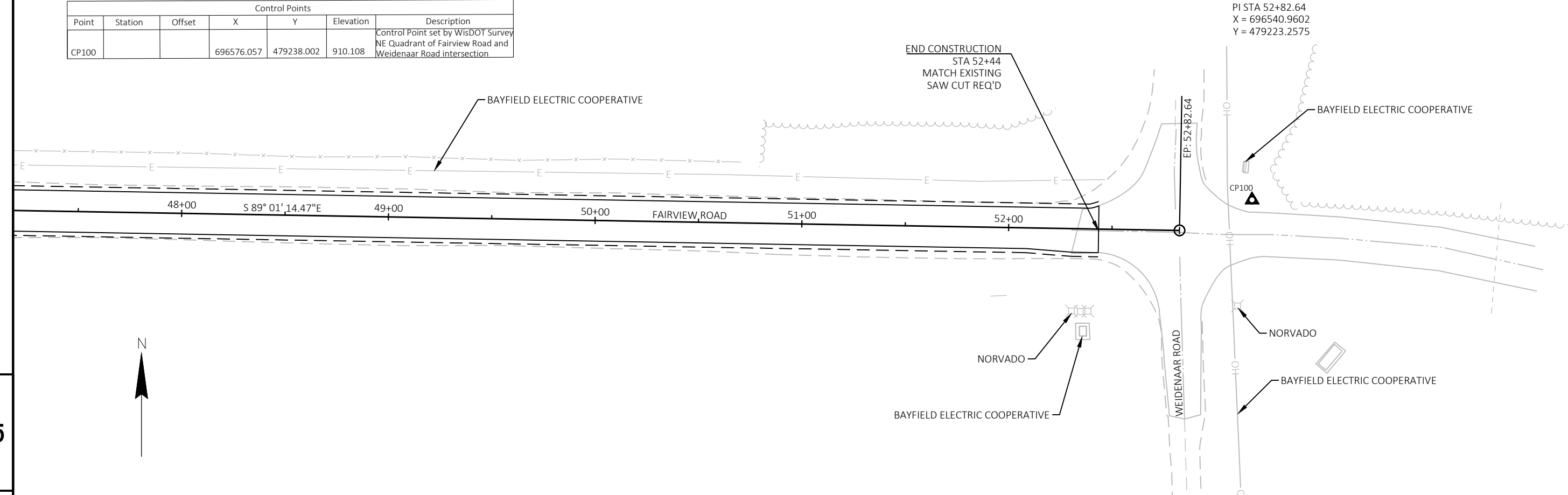


| Control Points | | | | | | |
|----------------|----------|---------|------------|------------|-----------|------------------------------------|
| Point | Station | Offset | X | Y | Elevation | Description |
| CP101 | 31+78.98 | -19.854 | 694437.942 | 479279.063 | 898.486 | Control Point set by WisDOT Survey |
| CP102 | 37+92.63 | 30.762 | 695050.637 | 479217.967 | 896.434 | Control Point set by WisDOT Survey |



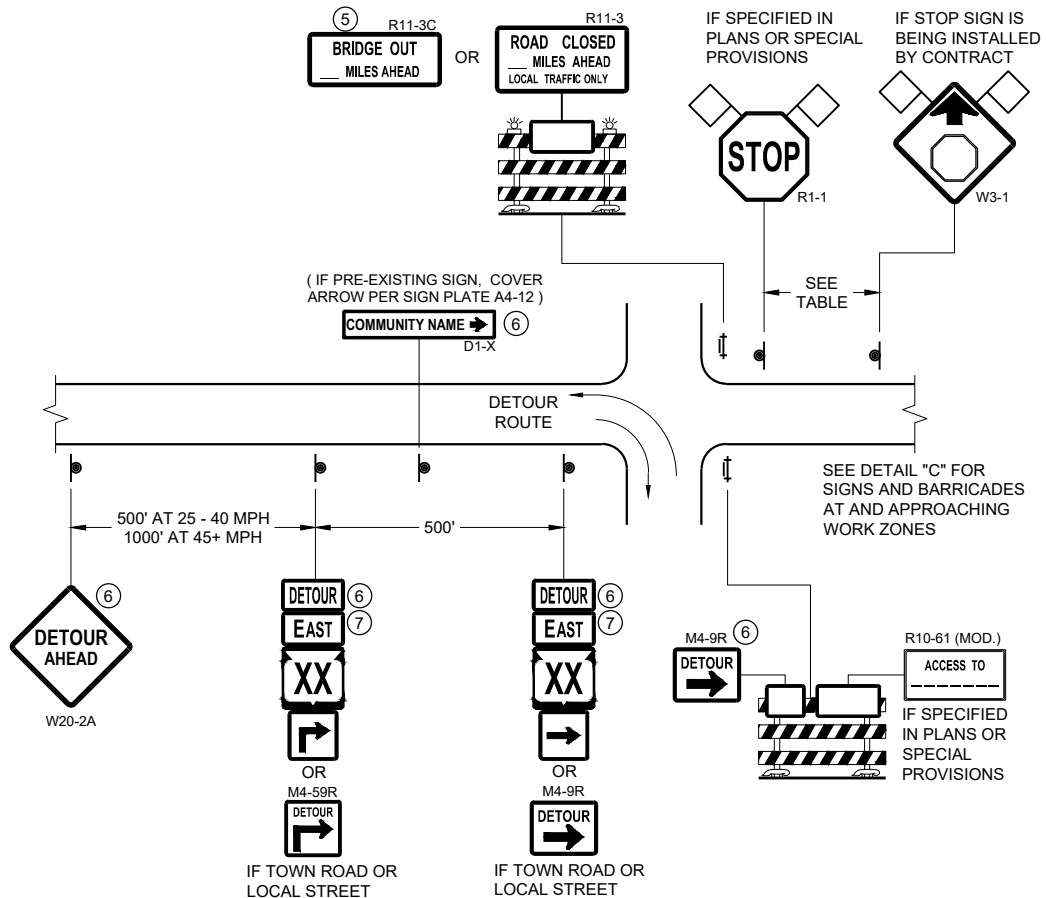
| | | | | | |
|------------------------|--------------------|------------------|-------------|-------|---|
| PROJECT NO: 8358-00-70 | HWY: FAIRVIEW ROAD | COUNTY: BAYFIELD | PLAN SHEETS | SHEET | E |
|------------------------|--------------------|------------------|-------------|-------|---|

| Control Points | | | | | | |
|----------------|---------|--------|------------|------------|-----------|---|
| Point | Station | Offset | X | Y | Elevation | Description |
| CP100 | | | 696576.057 | 479238.002 | 910.108 | Control Point set by WisDOT Survey NE Quadrant of Fairview Road and Weidenaar Road intersection |

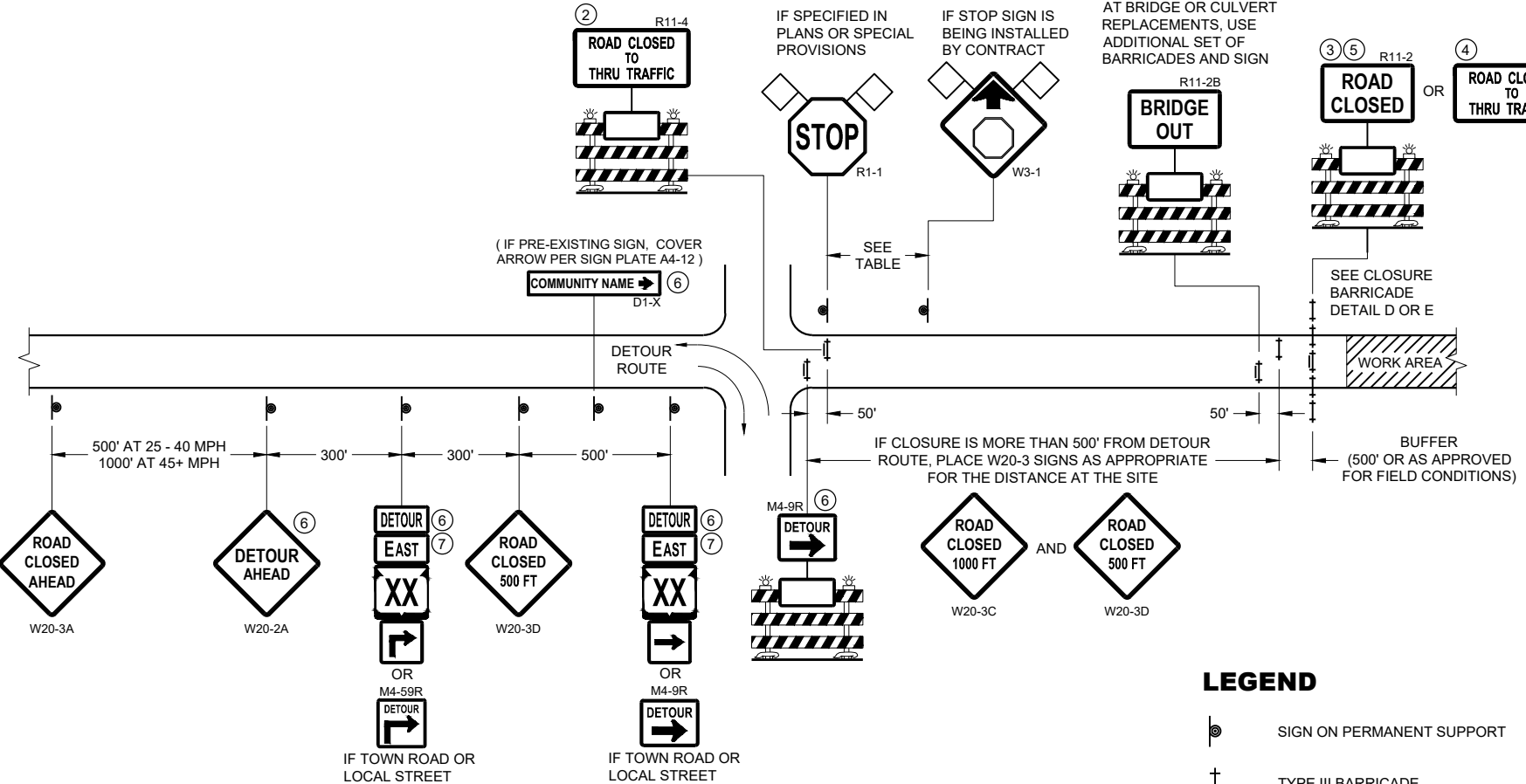


Standard Detail Drawing List

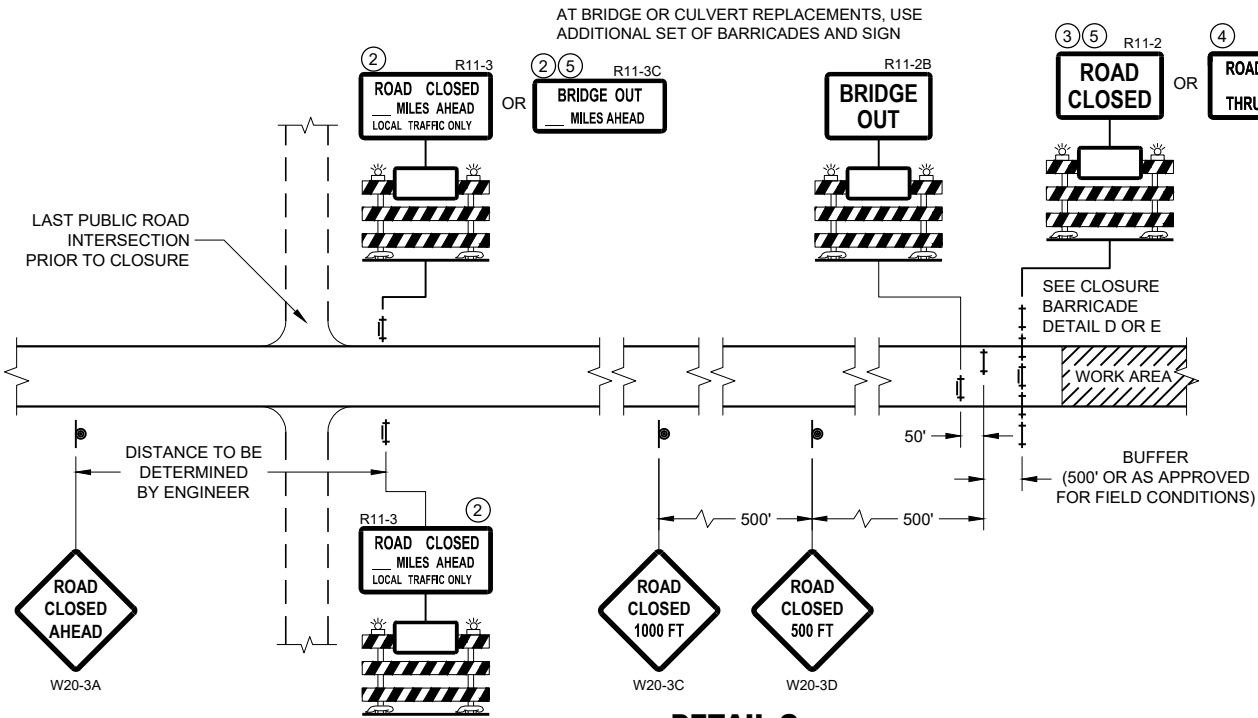
| | |
|-----------|--|
| 15C02-08A | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-08B | BARRICADES AND SIGNS FOR VARIOUS CLOSURES |
| 15C04-05 | TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC |
| 15C05-05 | TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS |
| 15C12-07 | TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION |
| 15D38-02A | TEMPORARY TRAFFIC CONTROL SIGN MOUNTING |
| 15D38-02B | ATTACHMENT OF SIGNS TO POSTS |



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE LESS THAN ½ MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

| SPEED LIMIT (MPH) | "STOP AHEAD" ADVANCE WARNING DISTANCE (FT) |
|-------------------|--|
| 25 | 200 |
| 30 | 200 |
| 35 | 350 |
| 40 | 350 |
| 45 | 500 |
| 50 | 550 |
| 55 | 750 |

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR M4 - 8
- EAST M3 - X
- XX M1 - 4 OR XX M1 - 6 OR COUNTY M1 - 5A
- M05 - 1 OR M06 - 1

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



SEE SDD 15C2 - SHEET "a" FOR LEGEND

ORIGIN SHALL BE 40' X 40' UNLESS OTHERWISE NOTED BELOW.

R11 - 2 SHALL BE 48" X 30"

R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"

M4 - 9 SHALL BE 30" X 24"

M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)

M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)

M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)

MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)

D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

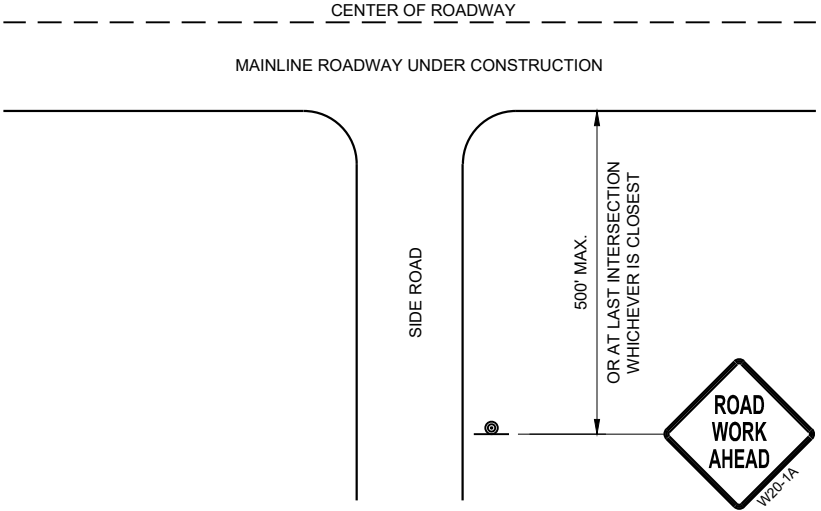
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

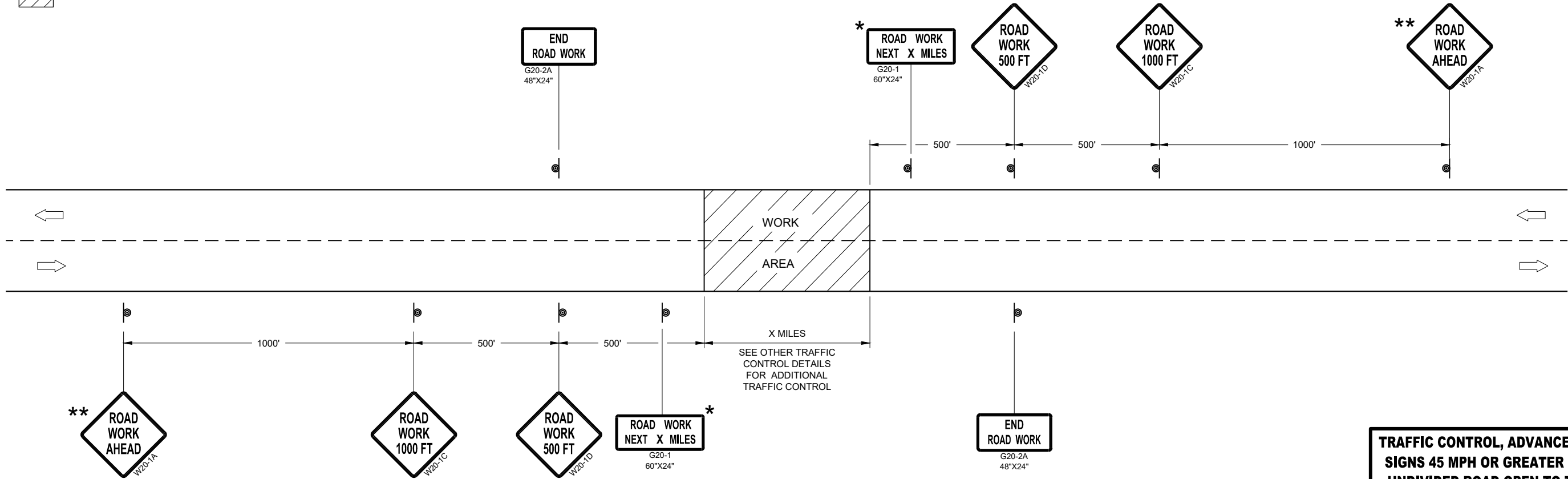
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

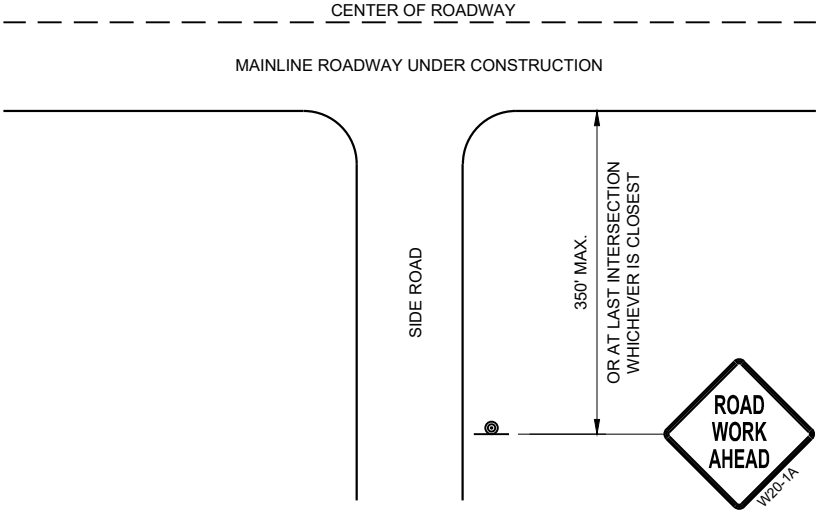
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

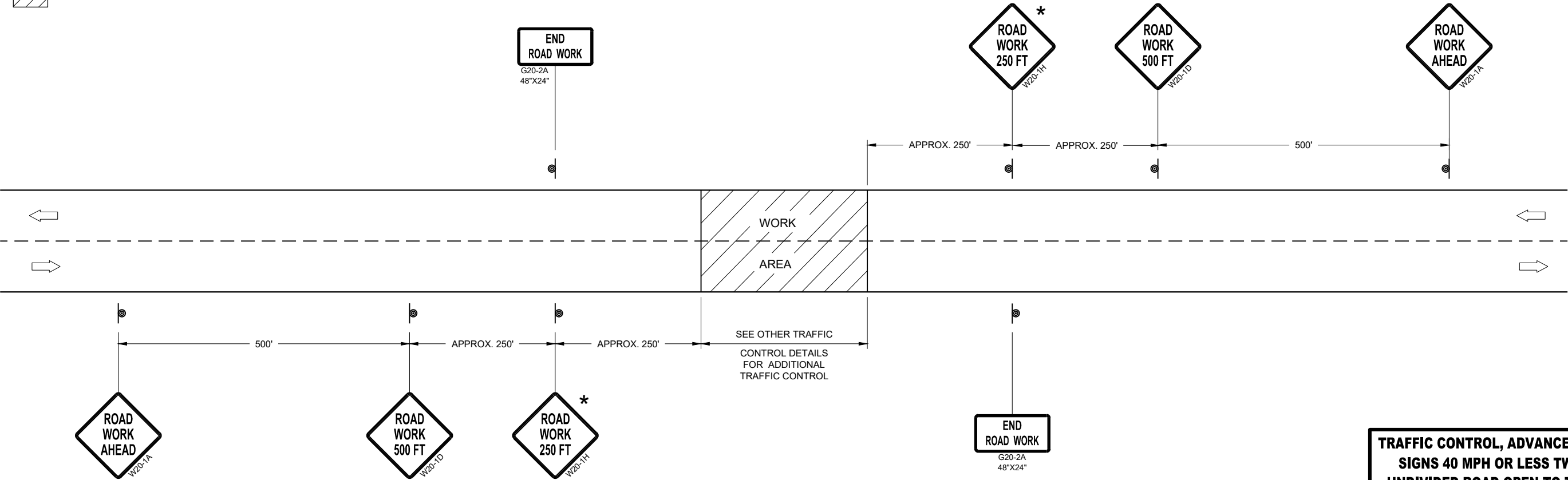
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS






TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

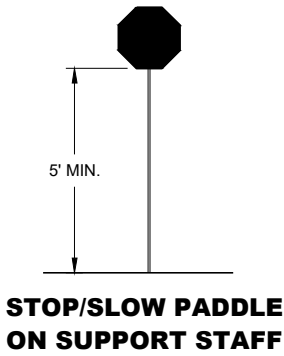
THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
- FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
 - SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.
- TEMPORARY PORTABLE RUMBLE STRIPS**
- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

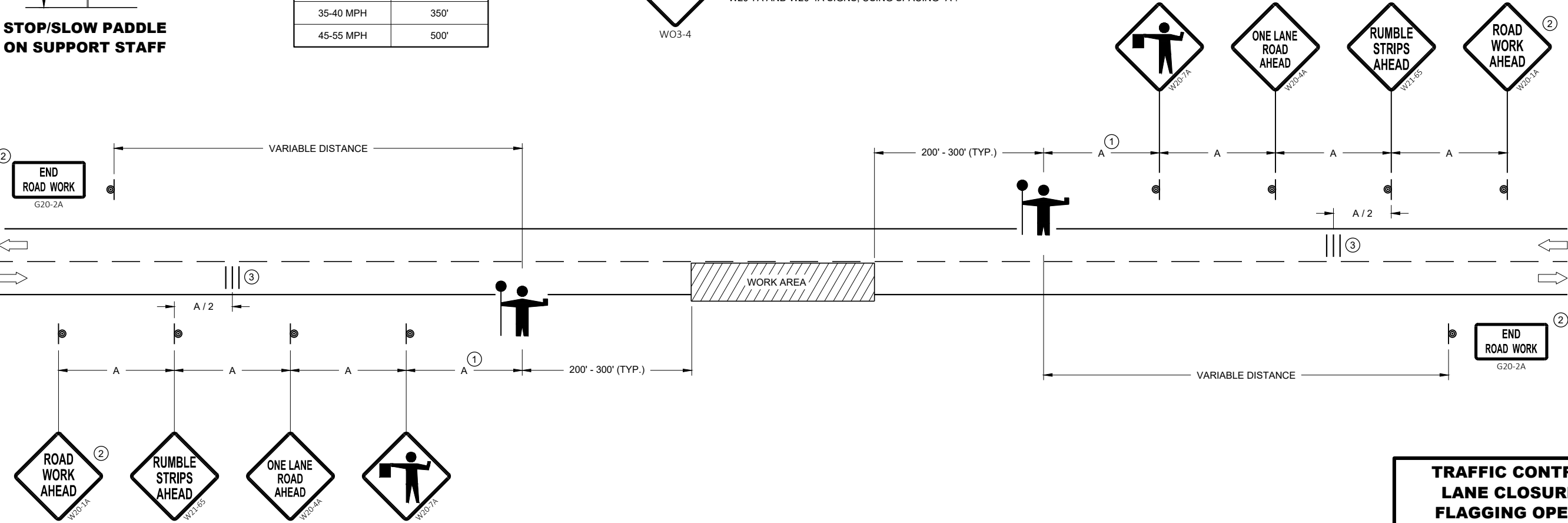


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

| SPEED LIMIT | SPACING "A" |
|-------------|-------------|
| 25-30 MPH | 200' |
| 35-40 MPH | 350' |
| 45-55 MPH | 500' |



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



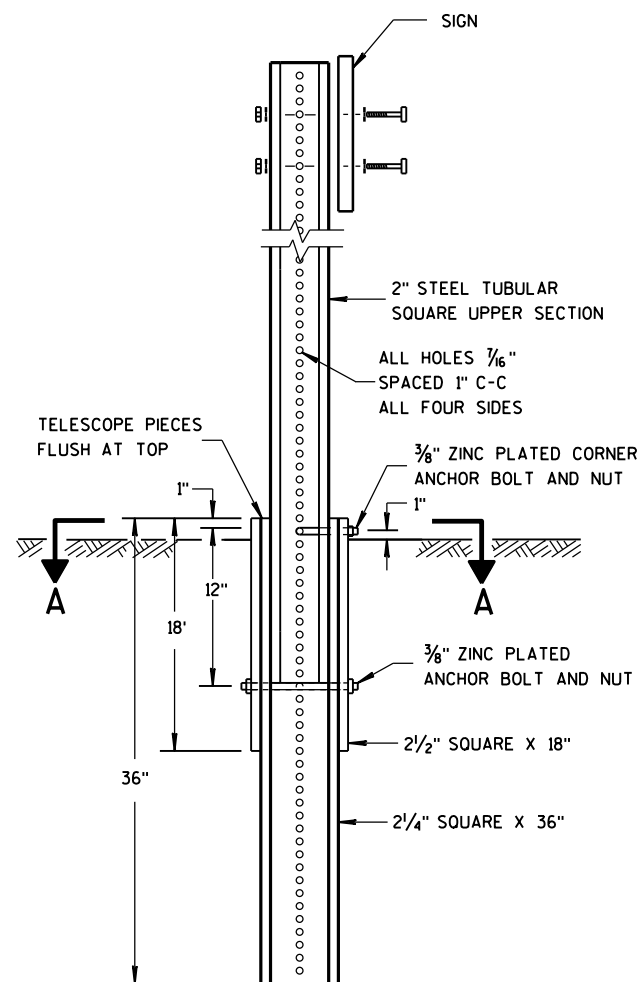
TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
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May 2019 /S/ Andrew Heidtke
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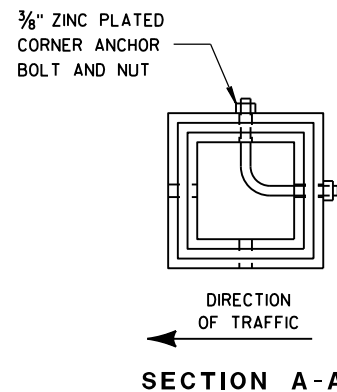
DETAIL OF TUBULAR
STEEL SIGN POST

TUBULAR STEEL POSTS

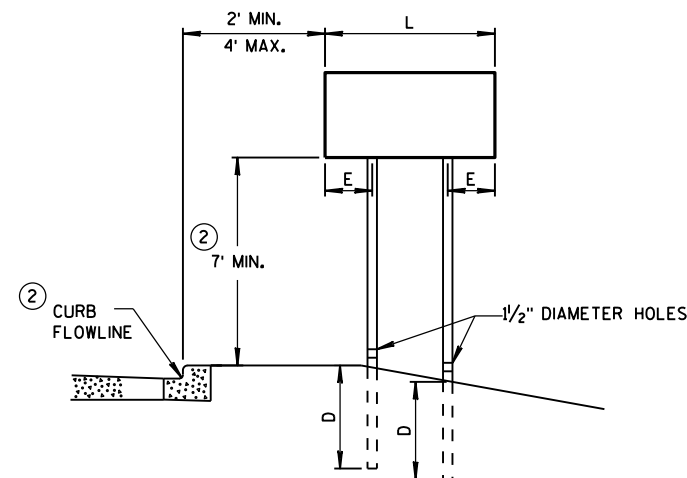
| AREA OF SIGN INSTALLATION (SQ. FT.) | NUMBER OF REQUIRED TUBULAR STEEL POSTS |
|--|--|
| 9 OR LESS | 1 |
| GREATER THAN 9 LESS THAN OR EQUAL TO 18 | 2 |
| GREATER THAN 18 LESS THAN OR EQUAL TO 27 | 3 |

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL
BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED
ON TUBULAR STEEL POSTS.



SECTION A-A

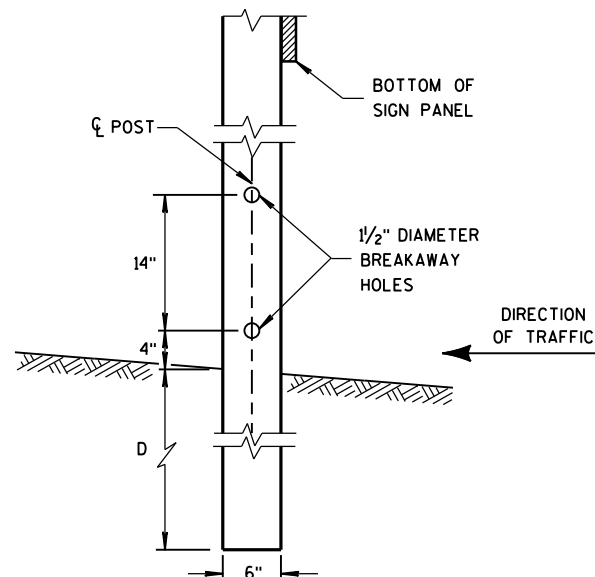


URBAN AREA

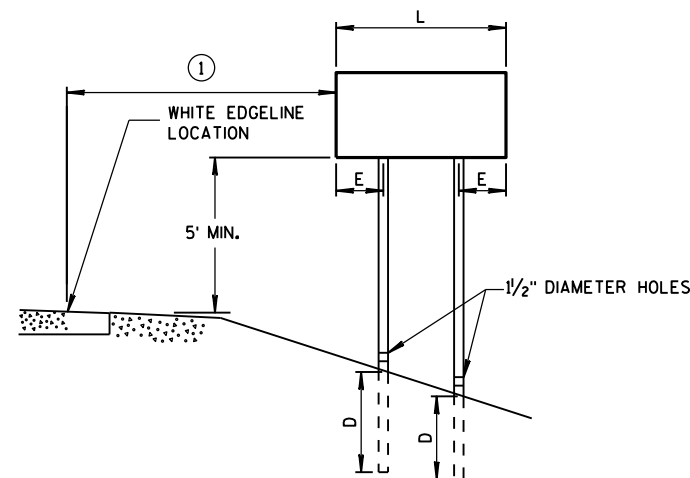
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST
EMBEDMENT DEPTH

| AREA OF SIGN INSTALLATION (SQ. FT.) | D (MIN) |
|---|------------|
| 20 OR LESS | 4' |
| GREATER THAN 20 | 5' |



4"x6" WOOD POST
MODIFICATION



RURAL AREA

4" X 6" WOOD POST

| POST SPACING REQUIREMENTS | | NUMBER OF WOOD POSTS REQUIRED |
|---|-----|-------------------------------------|
| L | E | |
| 48" OR LESS AND LESS THAN 20 SQ. FT. | - | 1 |
| LESS THAN 60" | 12" | 2 |
| 60" TO 120" | L/5 | 2 |
| GREATER THAN 120" LESS THAN 168" | 12" | 3 |
| 168" AND GREATER | 12" | 4 |

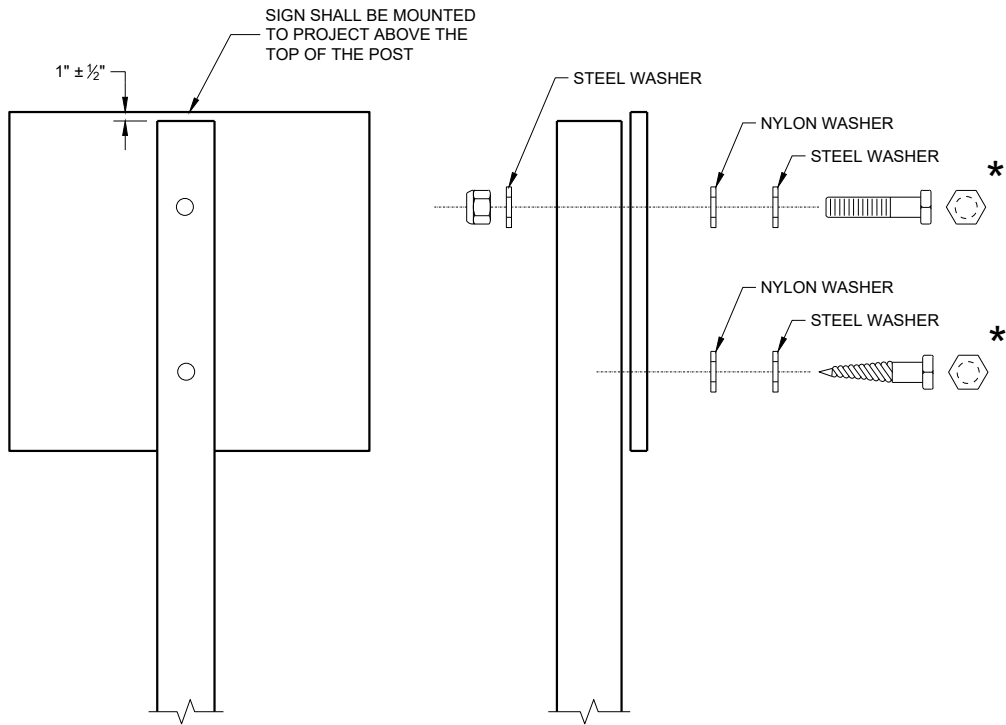
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL
SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS
SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM
DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM
DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH
SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED
COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POST (4" x 6")
LAG SCREWS - 3/8" x 3"
MACHINE BOLTS - 5/16" x 6 1/2" OR 7" LENGTH W/NUTS

SQUARE STEEL POST (2" x 2")
MACHINE BOLTS - 3/8" x 3 1/4" LENGTH W/NUTS
RIVETS - 3/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM
BODY/MANDREL O.D. FLANGE 0.720 - 0.765 INCH,
GRIP RANGE 0.042 - 0.375 INCH

WASHERS (ALL POSTS) -
1 1/4" O.D. x 3/8" I.D. x 1/16" STEEL
1 1/4" O.D. x 3/8" I.D. x 0.080 NYLON

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION
PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM
SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH
THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER
THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS
TO POSTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

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Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>