

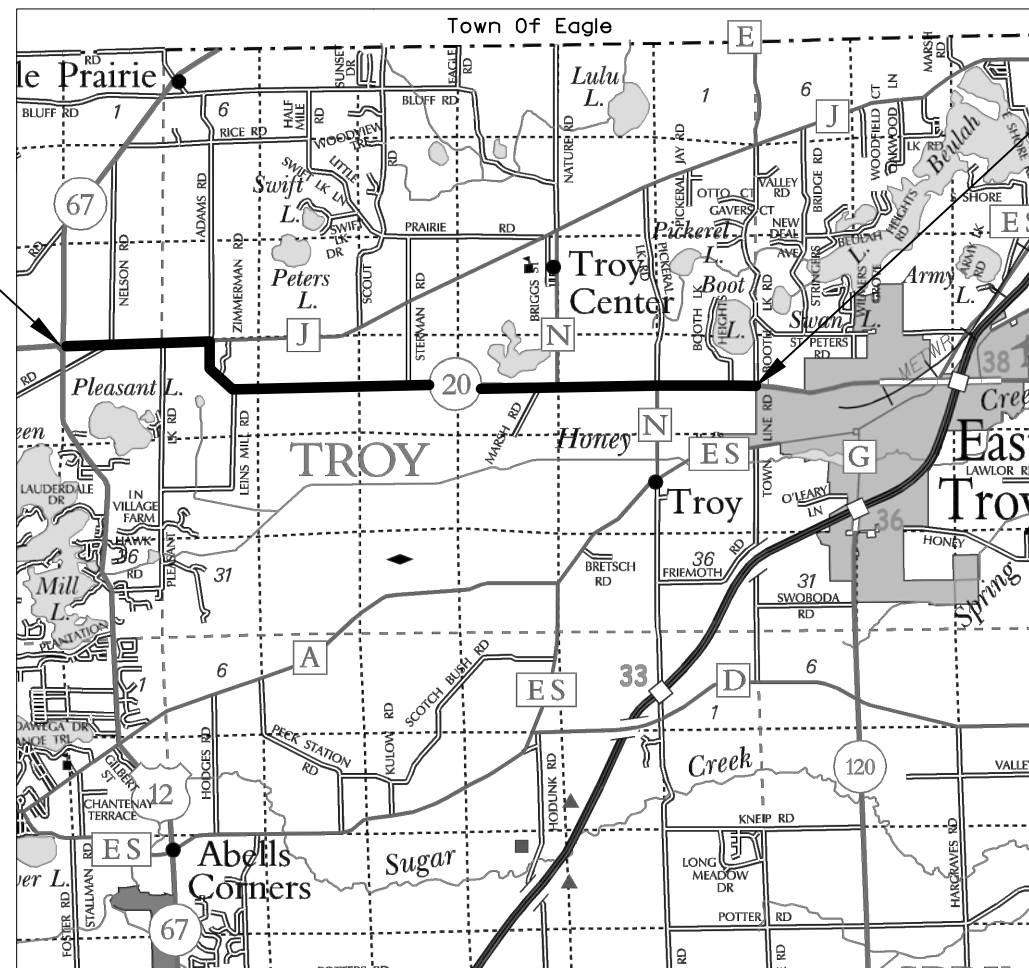
PLAN OF PROPOSED IMPROVEMENT

STH 67 TO TOWNLINE ROAD

WALWORTH COUNTY

STATE PROJECT NUMBER
3130-06-60

R-17-E



— END PROJECT 3130-06-60
STA 409+30

T-4-N

— 42° 45'

LAYOUT


SCALE 0 _____

TOTAL NET LENGTH OF CENTERLINE = 7.365 MI

"COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), WALWORTH COUNTY, NAD83 (2007) IN US SURVEY FEET."

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED
TO THE NORTH AMERICAN VERTICAL DATUM NAVD 88 (2007).

| STATE PROJECT | FEDERAL PROJECT | |
|---------------|-----------------|----------|
| | PROJECT | CONTRACT |
| 3130-06-60 | WISC 2020140 | 1 |
| | | |
| | | |
| | | |

| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
|----------------------------------------------------|------------------------------------------------------------------------------------------------------------------|
| PREPARED BY | |
| Surveyor | _____ WISDOT _____ |
| Designer | _____ JONATHAN ENGERSON _____ |
| Project Manager | _____ ALLEN GILBERTSON _____ |
| Regional Examiner | _____ _____ |
| Regional Supervisor | _____ JANET CANNON _____ |
| C.O. Examiner | _____ _____ |
| APPROVED FOR THE DEPARTMENT | |
| DATE: 4/30/2019 | _____  _____ (Signature) |

UTILITY AND GENERAL CONTACTS:

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PHONE: (414) 221-5617
FAX: (414) 221-2336
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CENTURYLINK
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MR. DERRIN WOLFORD, SIGNAL OPERATIONS LEAD WORK
WISDOT SIGNALS
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PO BOX 798
WAUKESHA, WI 53187 0798
PHONE: (262) 521-4409
DERRIN.WOLFORD@DOT.WI.GOV

MS. MICHELE STUTE, TOWN CLERK
TROY, TOWN OF
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EAST TROY, WI 53120
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FAX: (262) 642-5227
CLERKTREAS@TOWNOFTROY.COM

MR. STEVE CRAMER, UTILITY COORD.
CHARTER COMMUNICATIONS
1320 N. MARTIN LUTHER KING JR. DR.
MILWAUKEE, WI 53212
PHONE: (414) 277-4045
FAX: (414) 277-0638
WIS.ENGINEERING@CHARTER.COM

OTHER CONTACTS

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CRAIG WEBSTER
DNR SERVICE CENTER
141 NW BARSTOW STREET
ROOM 180
WAUKESHA, WI 53188
PHONE: (262) 574-2141
CRAIG.WEBSTER@WISCONSIN.GOV

SEWRPC CONTACT
JOHN WASHBURN
W239 N1812 ROCKWOOD DR.
PO BOX 1607
WAUKESHA, WI 53187-1607
PHONE : (262) 547-6721
CELL: (262) 953-4295
JWASHBURN@SEWRPC.ORG

HMA PAVING

2" HMA PAVEMENT
2" - 4LT 58-28 S UPPER LAYER

6" HMA BASE PATCHING ASPHALTIC
HMA ASPHALTIC SURFACE TO BE PLACED WITH MAX OF 6" LIFTS

GENERAL NOTES

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT LOCATION THAT ARE NOT SHOWN.

THE LOCATIONS OF JOINTS IN THE HMA PAVEMENT SHALL BE APPROVED BY THE ENGINEER.

THE CONTRACTOR WILL MAINTAIN ACCESS TO ALL DRIVEWAYS AND BUSINESSES AT ALL TIMES.

VERTICAL BUTT JOINTS WILL BE REQUIRED WHERE NEW PAVEMENT IS TO MEET AN EXISTING PAVED SURFACE. BUTT JOINTS SHALL BE INCIDENTAL TO ASPHALTIC SURFACE MILLING AND WILL NOT BE PAID SEPERATELY AS SAWED JOINTS.

TRAFFIC CONTROL LOCATIONS AS SHOWN IN THE PLAN ARE SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

ALL SIGN LOCATIONS SHALL BE REVIEWED BY THE ENGINEER PRIOR TO INSTALLATION.

NO TREES OR SHRUBS SHALL BE REMOVED UNLESS DESIGNATED FOR REMOVAL BY THE ENGINEER.

COORDINATE WITH SOUTHEASTERN WISCONSIN REGIONAL PLANNING COMMISSION (SEWRPC) FOR THE PERPETUATION OF SECTION CORNER (PUBLIC LAND SURVEY SYSTEM - PLSS) MONUMENTS. THE CONTRACTOR IS RESPONSIBLE TO COORDINATE WITH SEWRPC AND THE WISDOT PROJECT MANAGER THROUGHOUT THE PERPETUATION PROCESS. THE CONTRACTOR WILL CONTACT SEWRPC AT (262) 953-4295 AT LEAST TWO WEEKS BEFORE STARTING CONSTRUCTION OPERATIONS OR THE PRECONSTRUCTION MEETING TO ALLOW FOR SECTION CORNER MONUMENT PERPETUATION COORDINATION.

SECTION CORNERS WITHIN THE ROADWAY WILL BE ADDRESSED WITH PAVING RINGS OR FULL DEPTH REPLACEMENT OF THE MONUMENT. THE SEWRPC REPRESENTATIVE WILL MAKE THE DETERMINATION OF THE EXTENT OF THE WORK INVOLVED WITH THE SECTION CORNER MONUMENTS BASED ON INSPECTION DURING CONSTRUCTION.





PROJECT NO: 3130-06-60

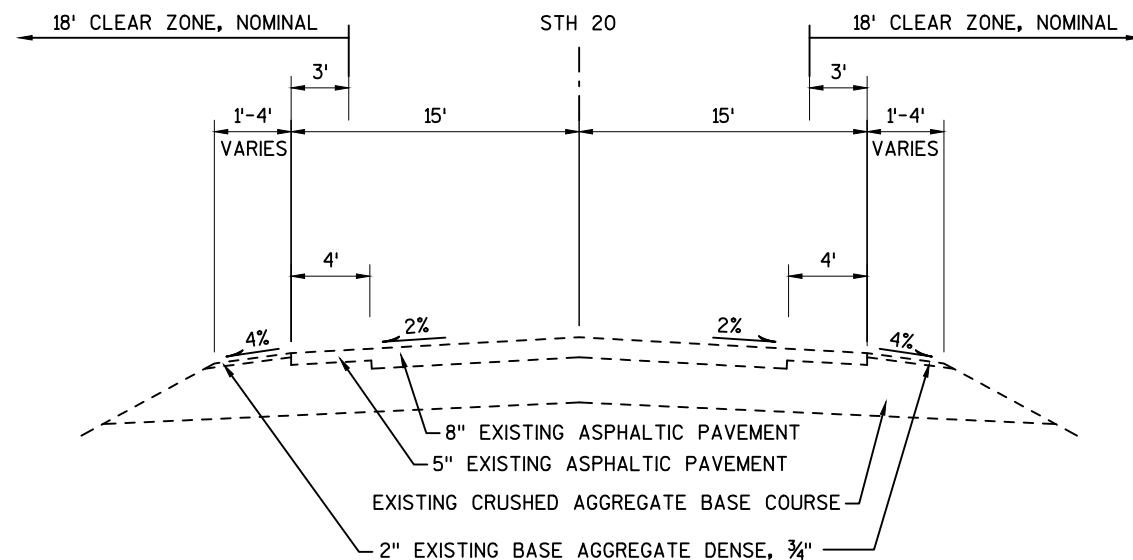
HWY: STH 20

COUNTY: WALWORTH

PROJECT OVERVIEW

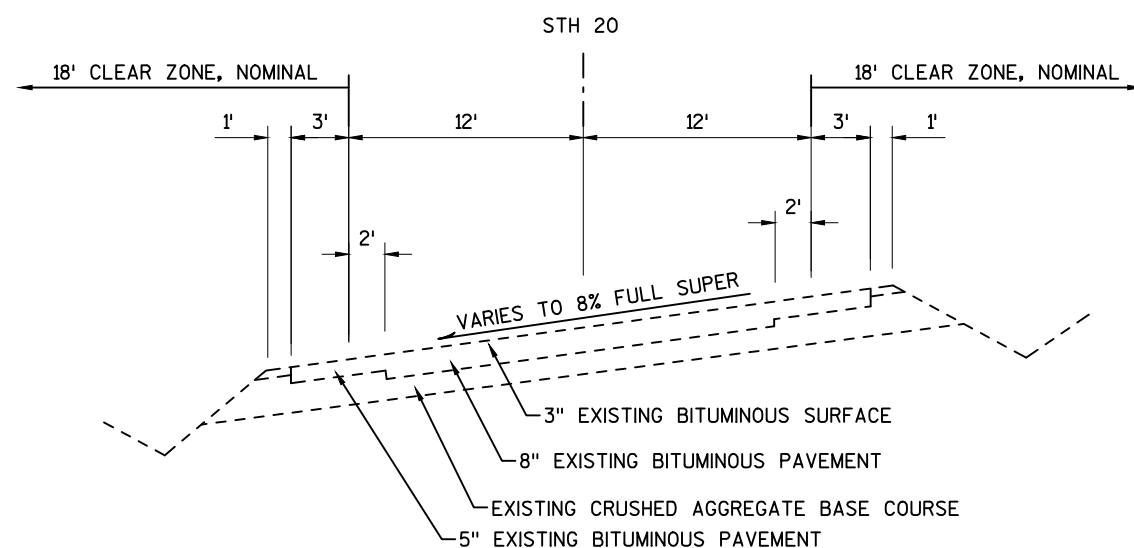
SHEET

E



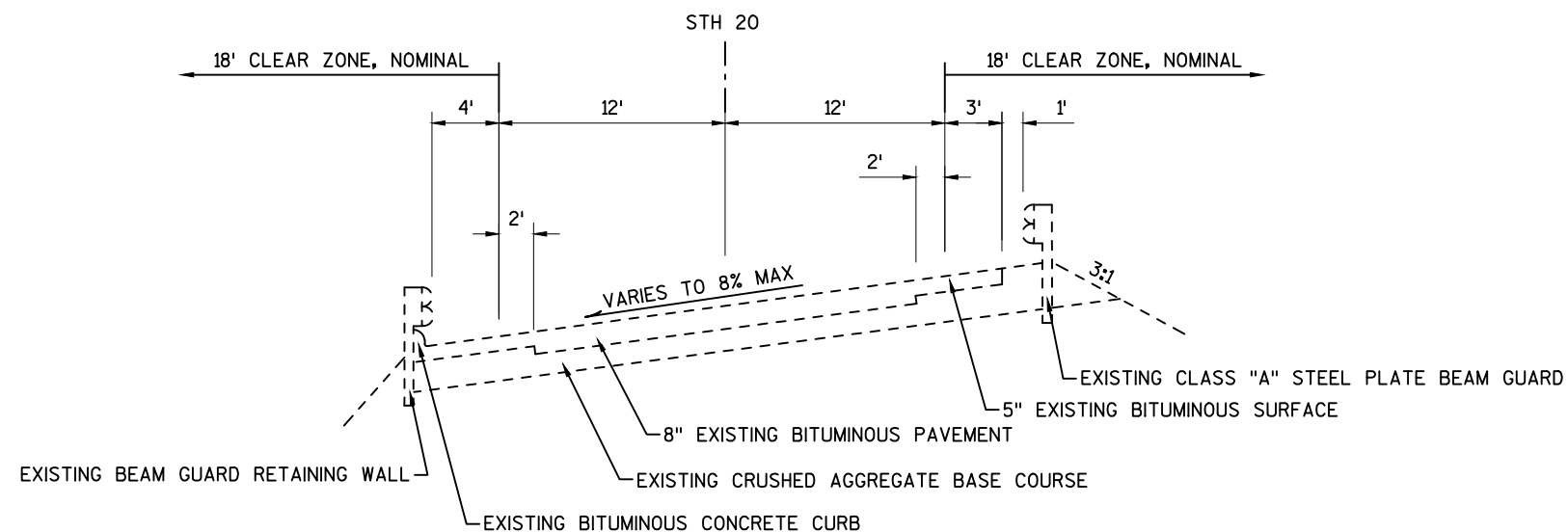
TYPICAL EXISTING SECTION
STH 20 - (GENERAL)

STH 67 TO TOWNLINE ROAD
STA 20+00 - STA 403+00



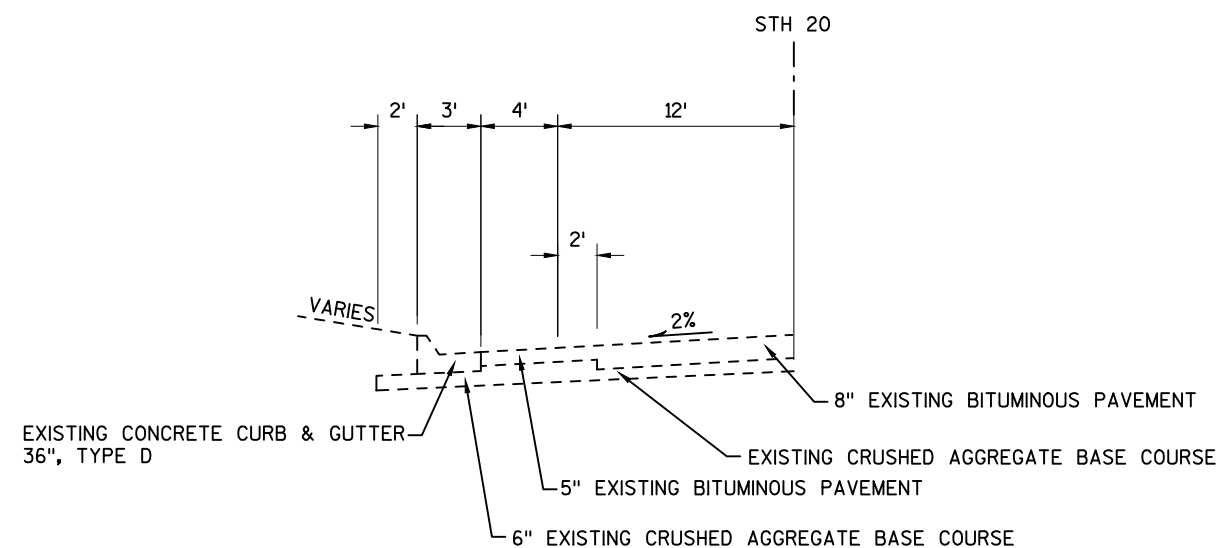
TYPICAL EXISTING SECTION
STH 20

STA 125+92.90 - STA 134+54.0



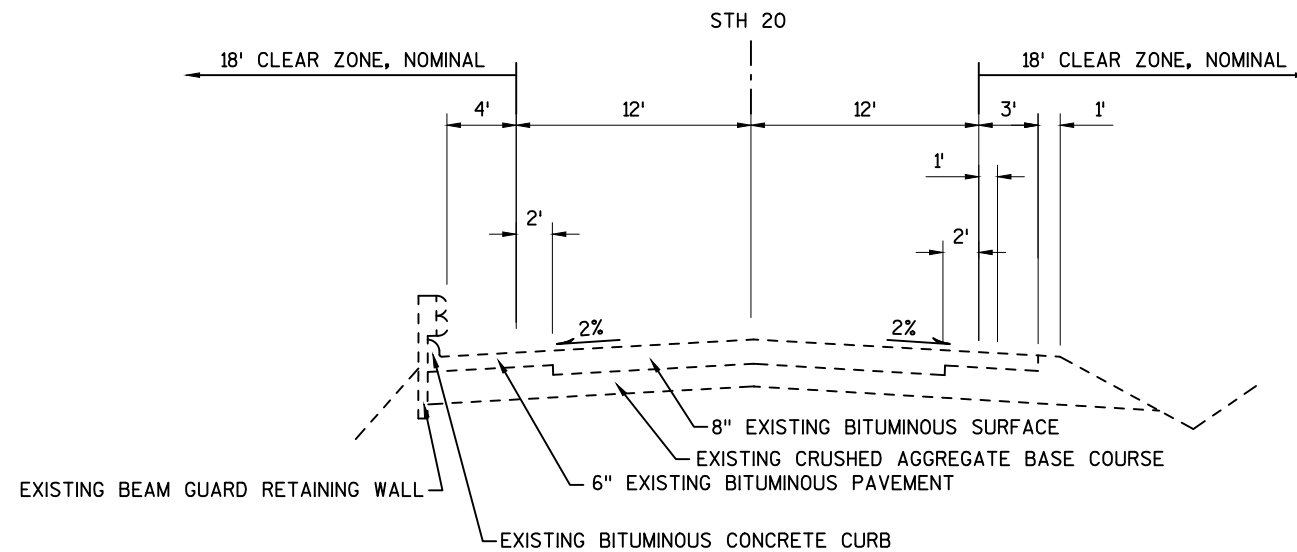
TYPICAL EXISTING SECTION
STH 20

STA 111+00 - STA 115+00
TRANSITIONS FROM STA 110+00 - STA 111+00
STA 115+00 - STA 116+00



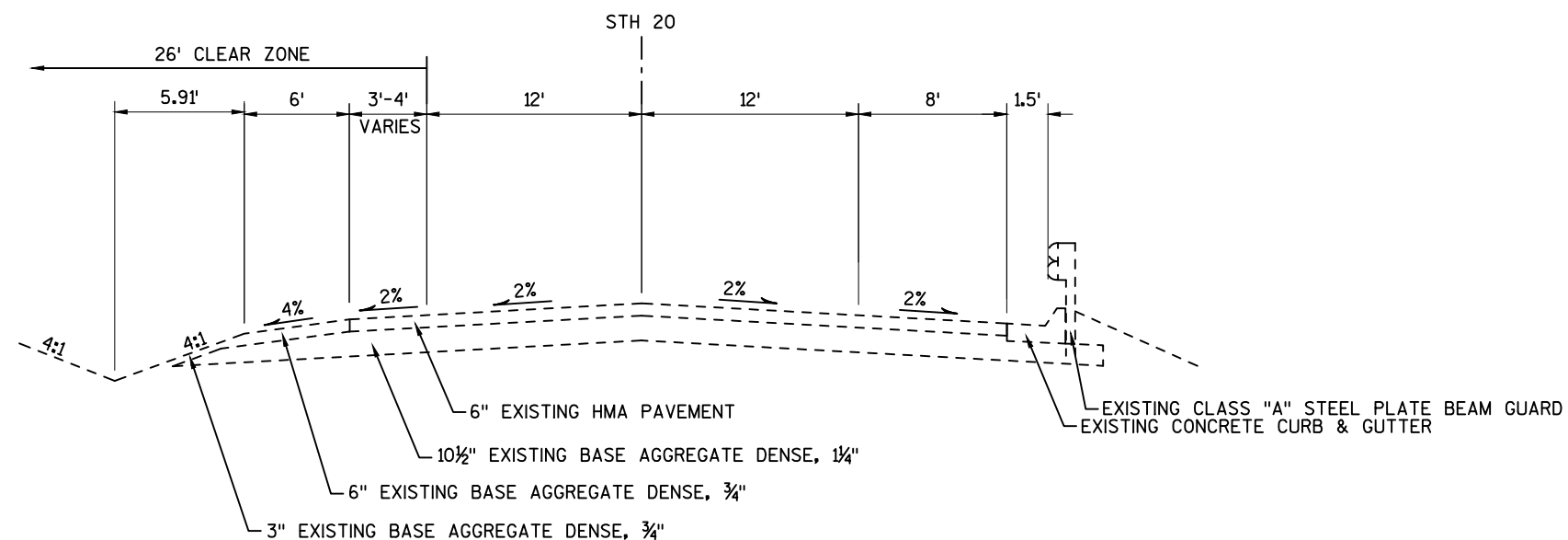
TYPICAL EXISTING SECTION (HALF SECTION)
STH 20 - CURB & GUTTER IN CUT AREAS

STA 233+00 - STA 236+50, LT
STA 233+00 - STA 237+25, RT
STA 239+75 - STA 240+75, RT
STA 241+50 - STA 246+00, LT
STA 268+00 - STA 273+00, RT
STA 272+00 - STA 273+00, LT
STA 396+00 - STA 398+75, RT



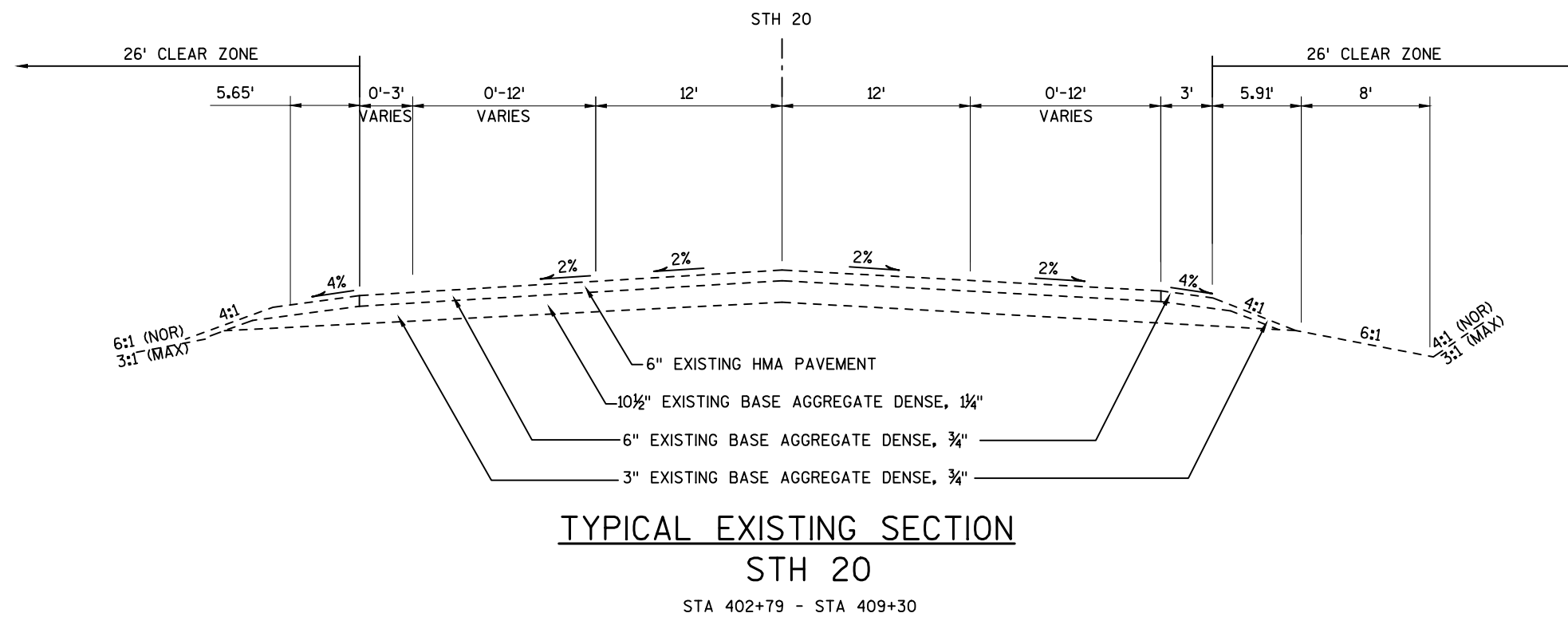
TYPICAL EXISTING SECTION STH 20

STA 237+30 - STA 240+00
TRANSITION SHOULDER WIDTHS:
STA 240+00 - STA 241+00
STA 236+30 - STA 237+30

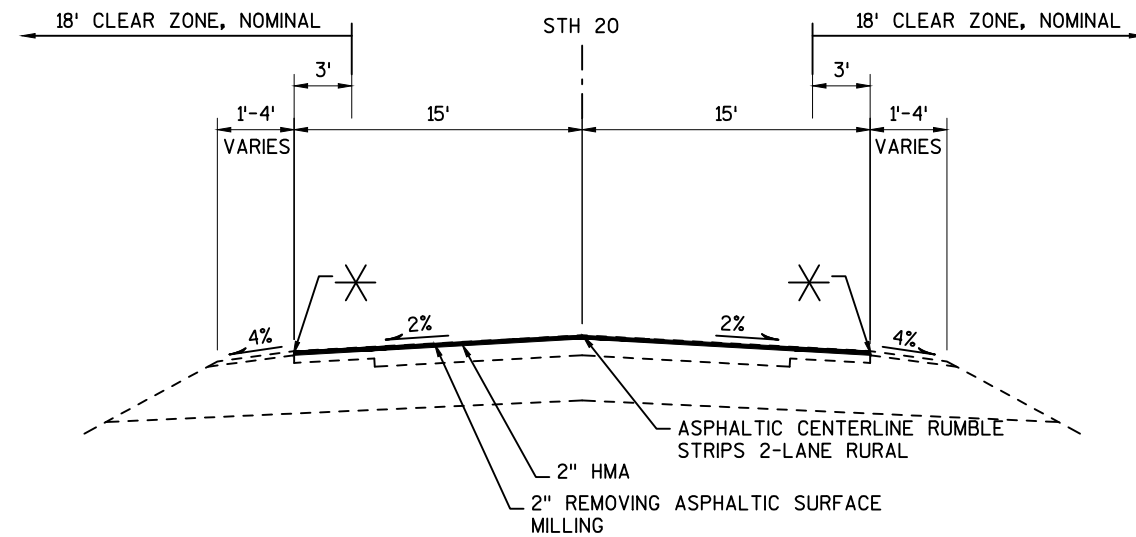


TYPICAL EXISTING SECTION STH 20

STA 395+00 - STA 400+25

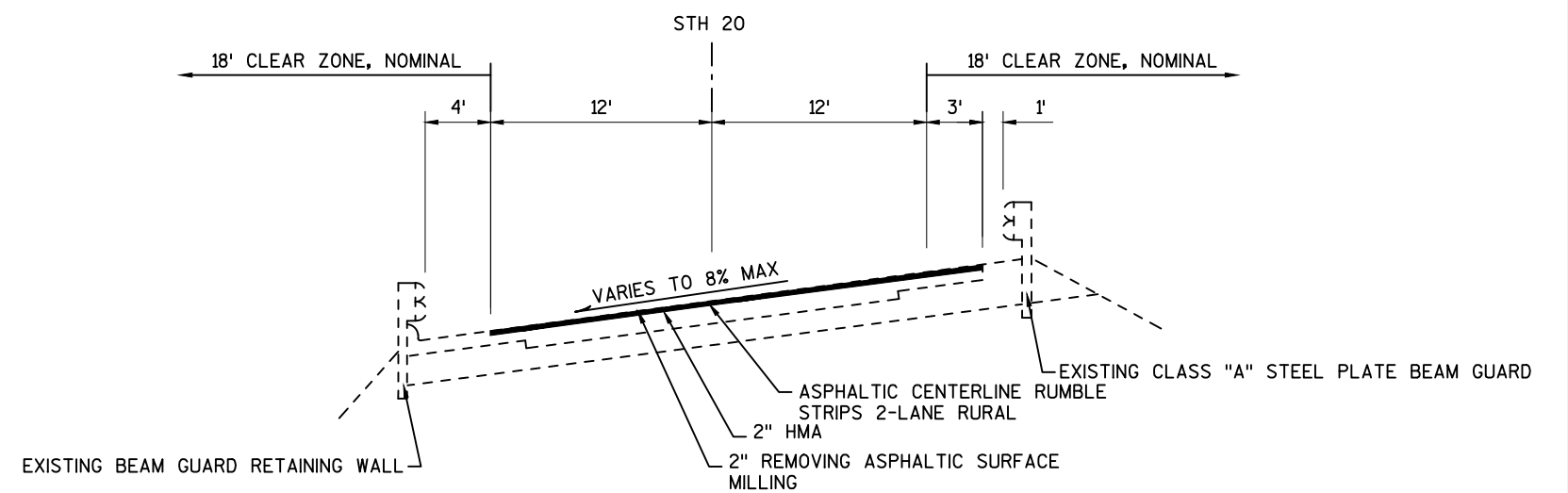


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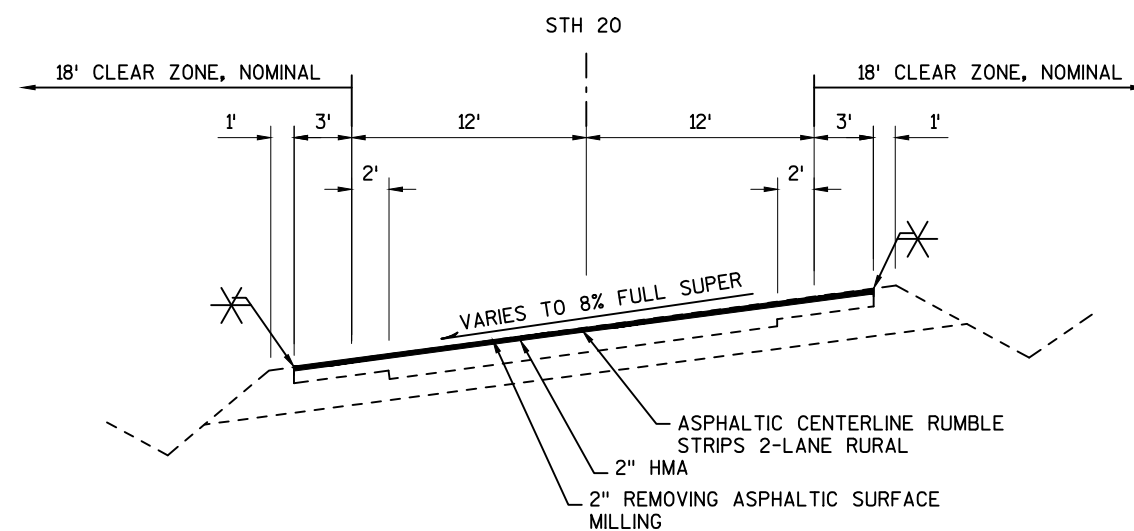
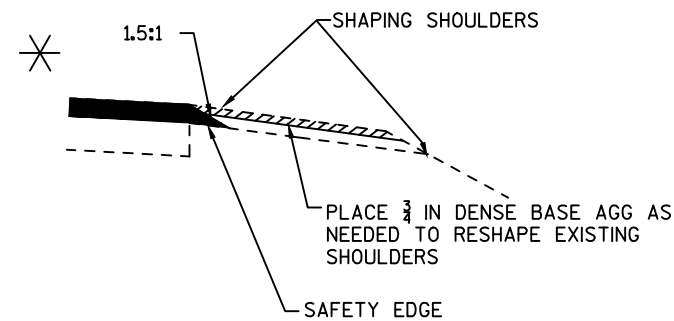
TYPICAL PROPOSED SECTION
STH 20 - (GENERAL)

STH 67 TO TOWNLINE ROAD
STA 20+45 - STA 403+00



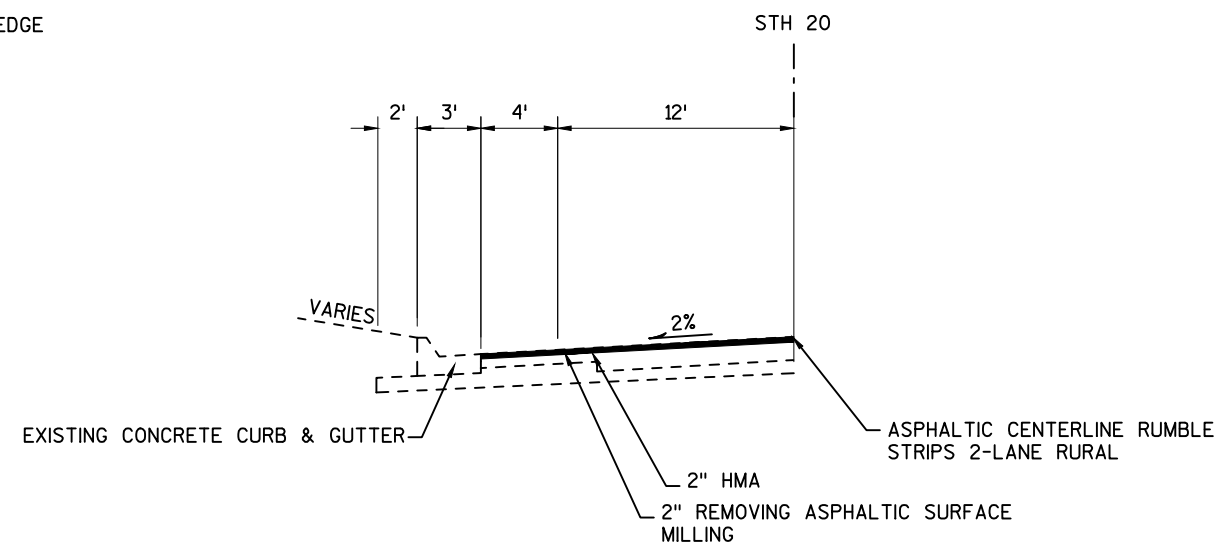
TYPICAL PROPOSED SECTION
STH 20

STA 111+00 - STA 115+00
TRANSITIONS FROM STA 110+00 - STA 111+00
STA 115+00 - STA 116+00



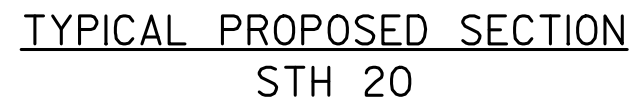
TYPICAL PROPOSED SECTION
STH 20

STA 125+93 - STA 134+54



TYPICAL PROPOSED SECTION (HALF SECTION)
 STH 20 - CURB & GUTTER IN CUT AREAS

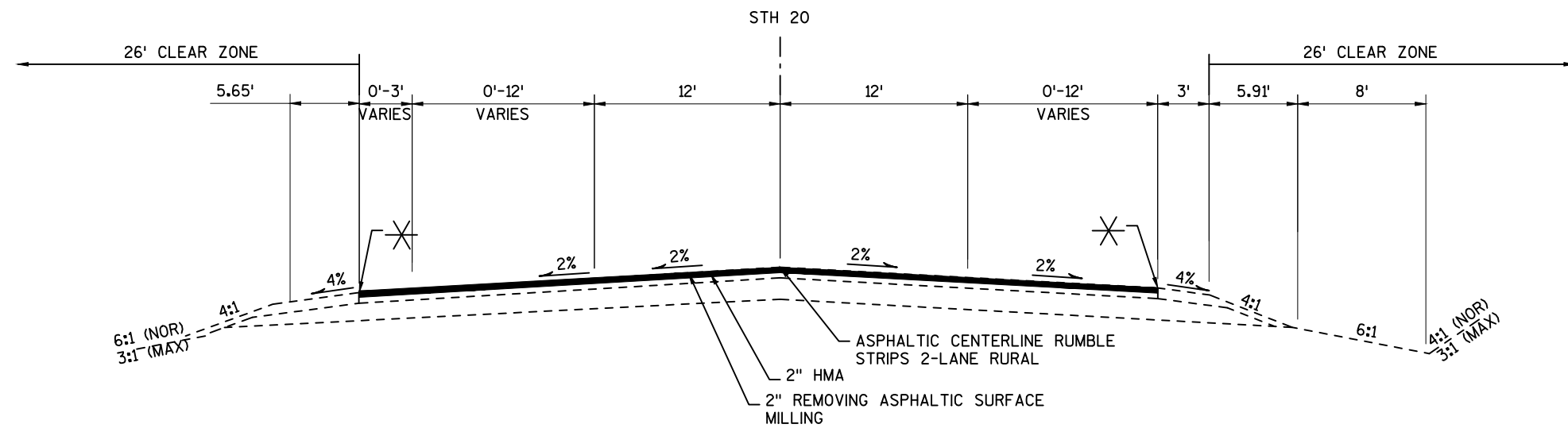
STA 233+00 - STA 236+50, LT
STA 233+00 - STA 237+25, RT
STA 239+75 - STA 240+75, RT
STA 241+50 - STA 246+00, LT
STA 268+00 - STA 273+00, RT
STA 272+00 - STA 273+00, LT
STA 396+00 - STA 398+75, RT



STA 236+30 - STA 237+30

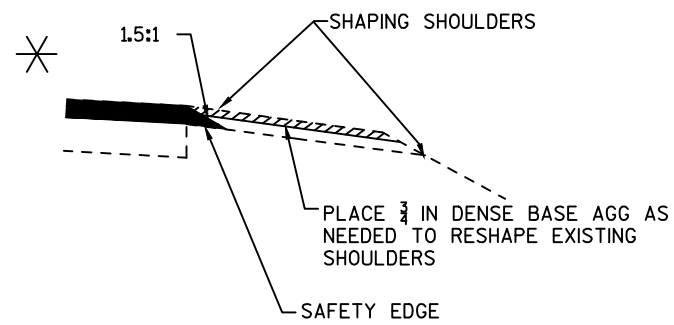


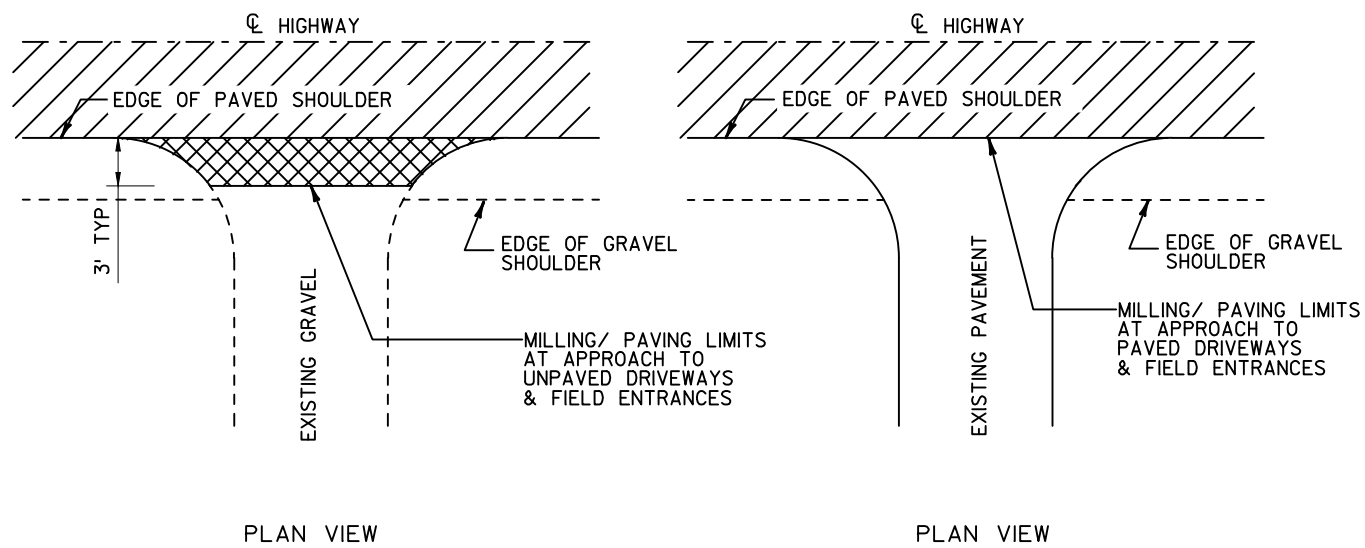
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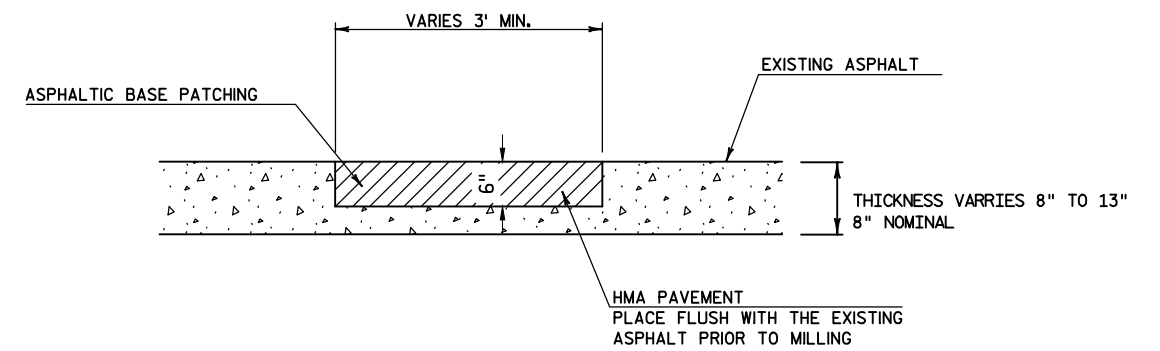
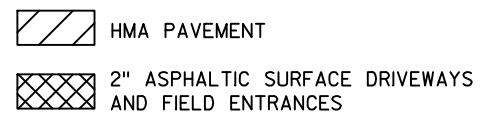
TYPICAL PROPOSED SECTION
STH 20

STA 402+79 - STA 409+30

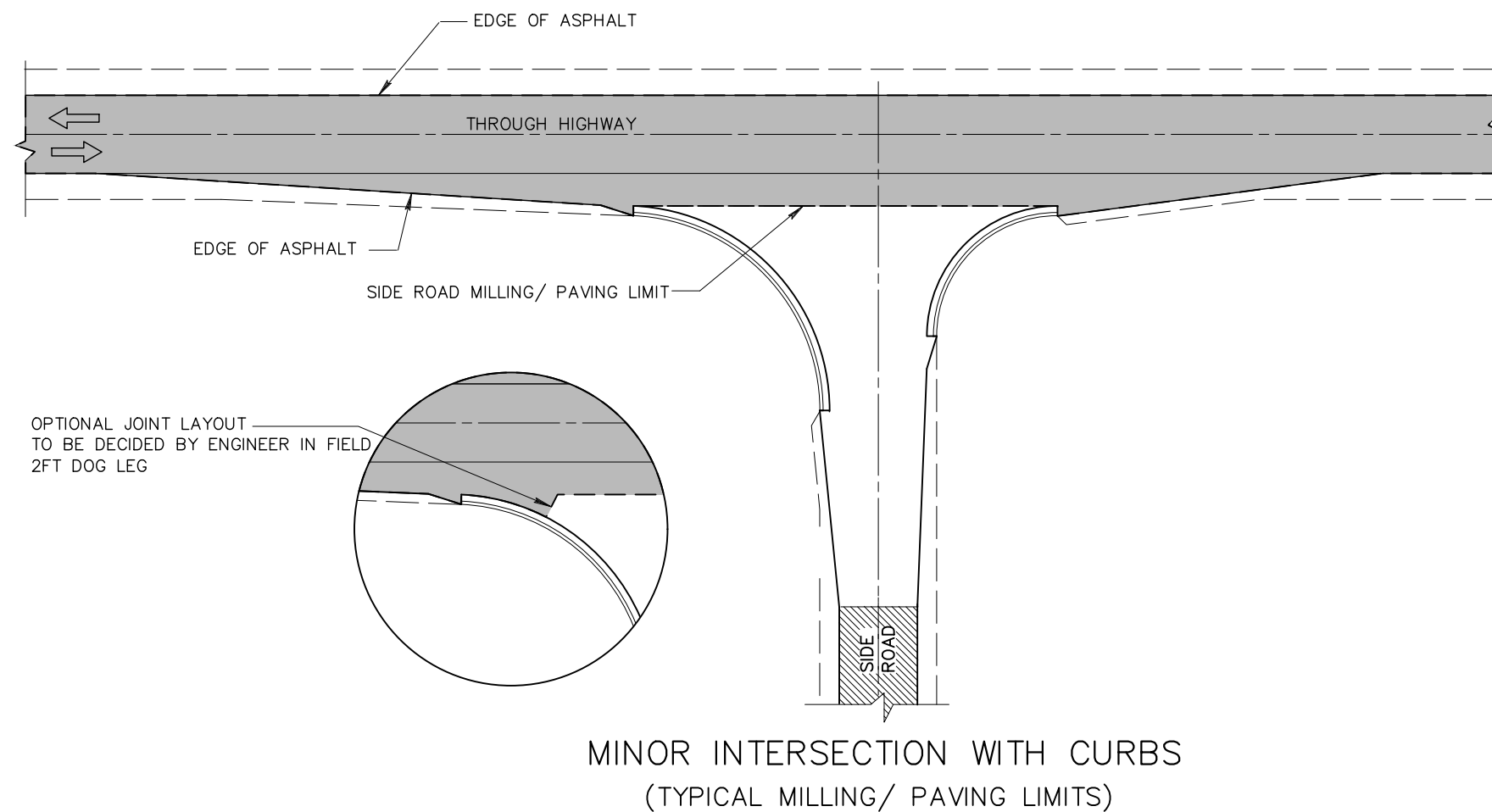
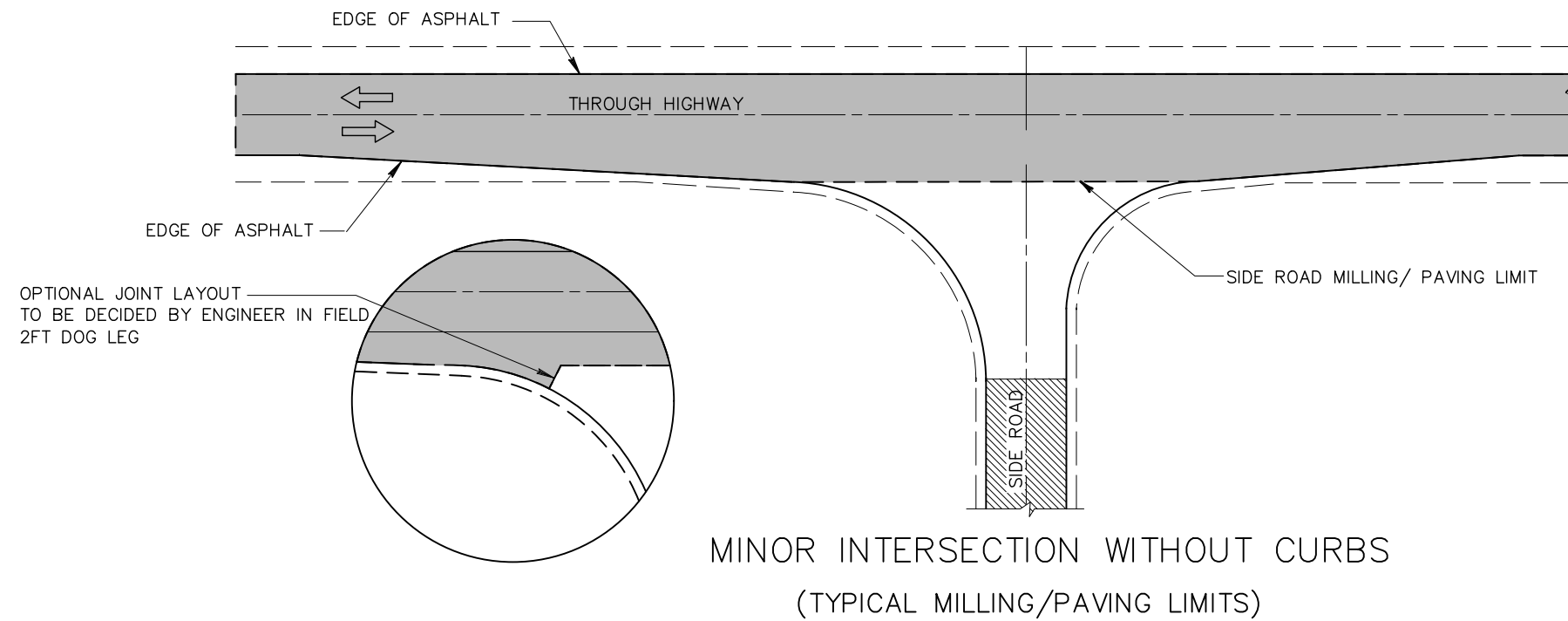


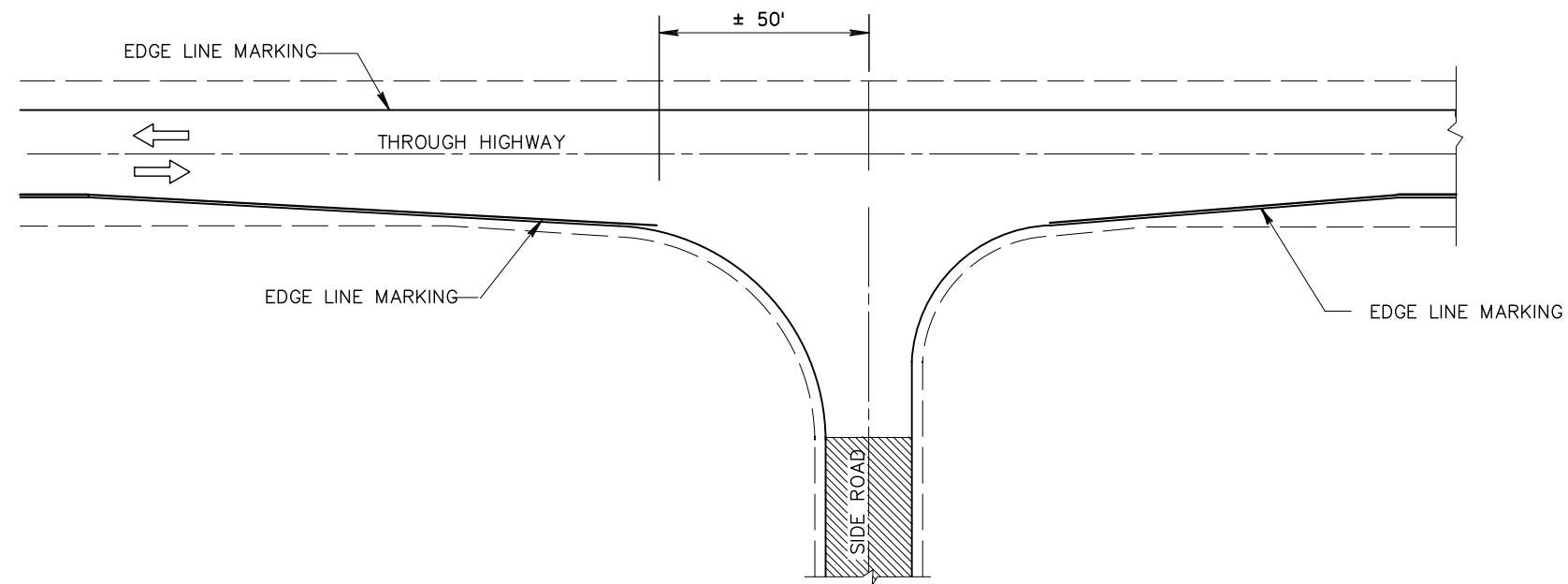


RURAL DRIVEWAY DETAIL

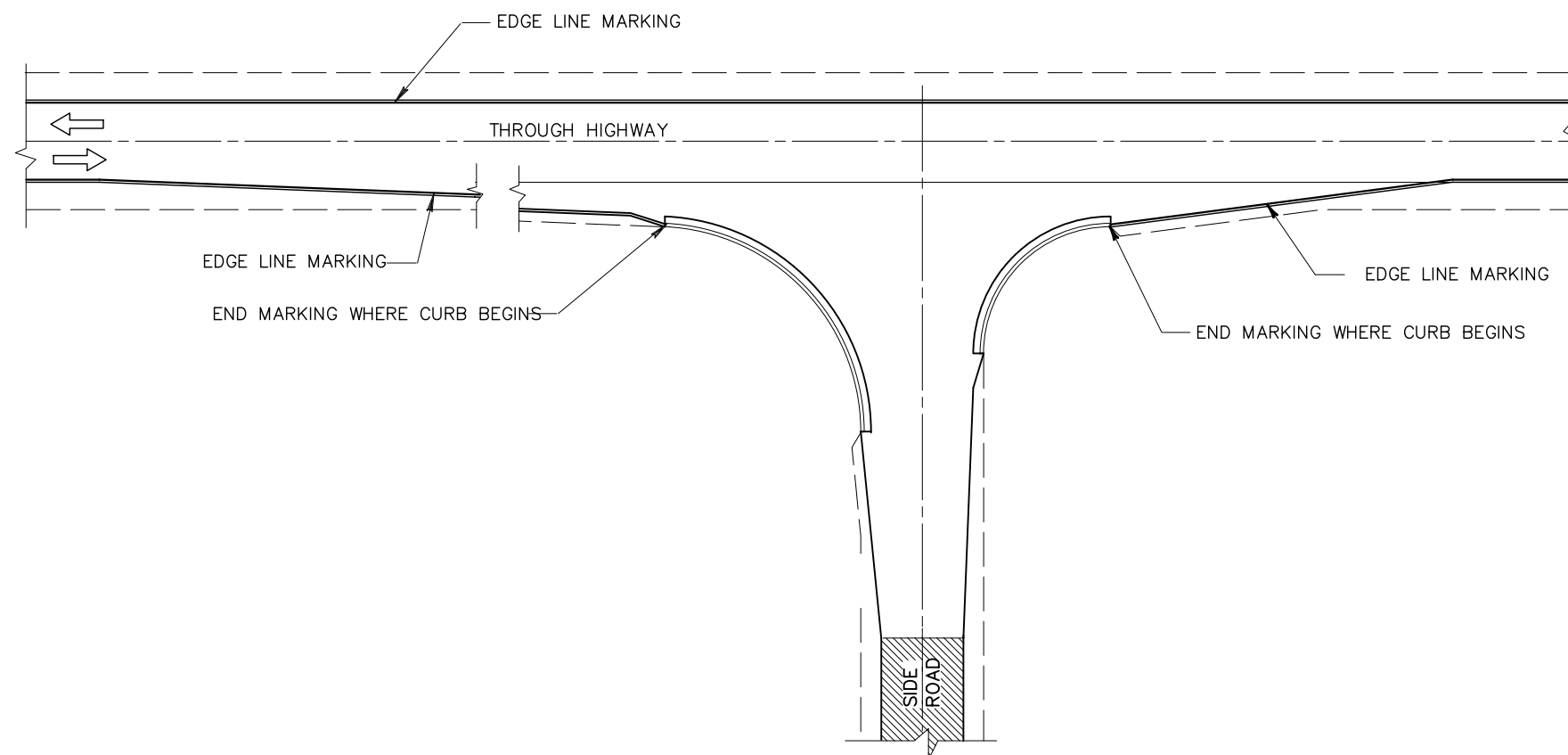


BASE PATCHING ASPHALTIC DETAIL (TYPICAL)
LOCATION TO BE DETERMINED BY THE ENGINEER IN THE FIELD

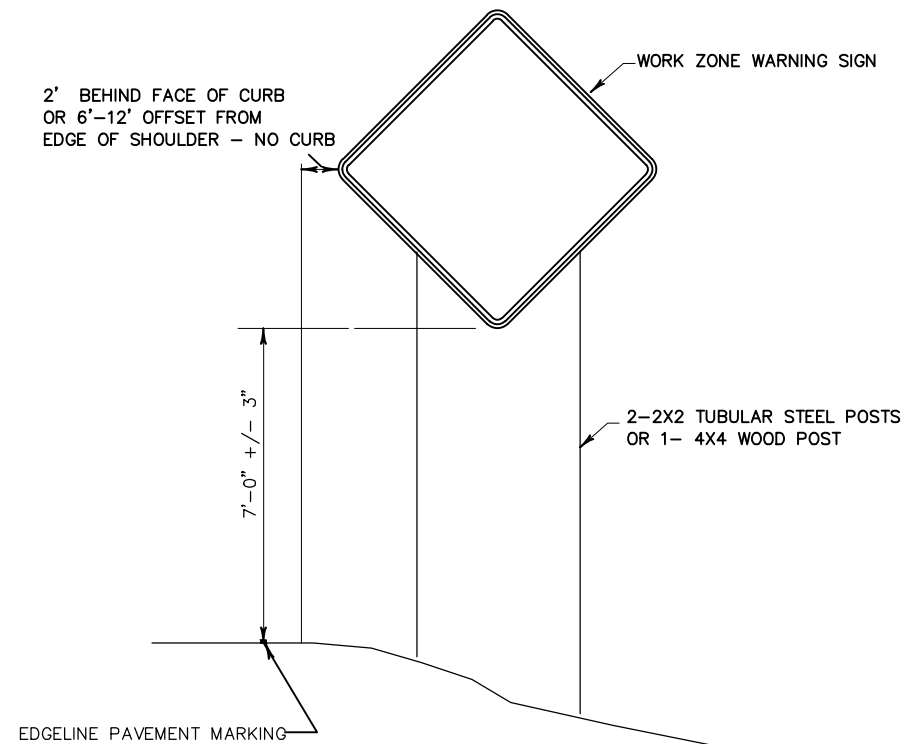




MINOR INTERSECTION WITHOUT CURBS
(TYPICAL MARKING)



MINOR INTERSECTION WITH CURBS
(TYPICAL MARKING)

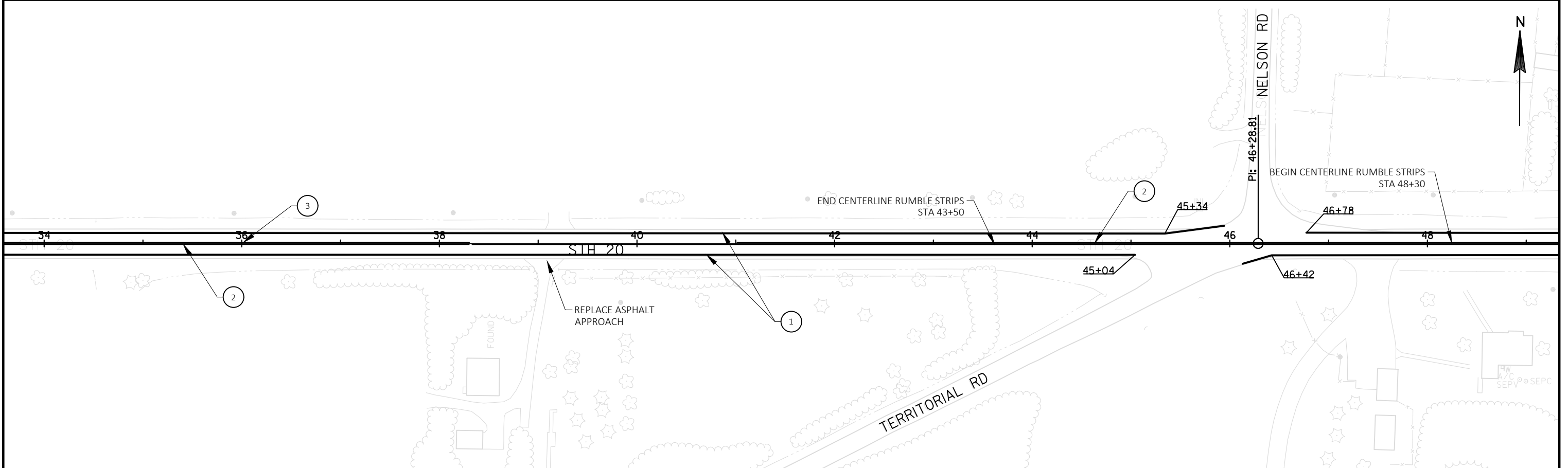
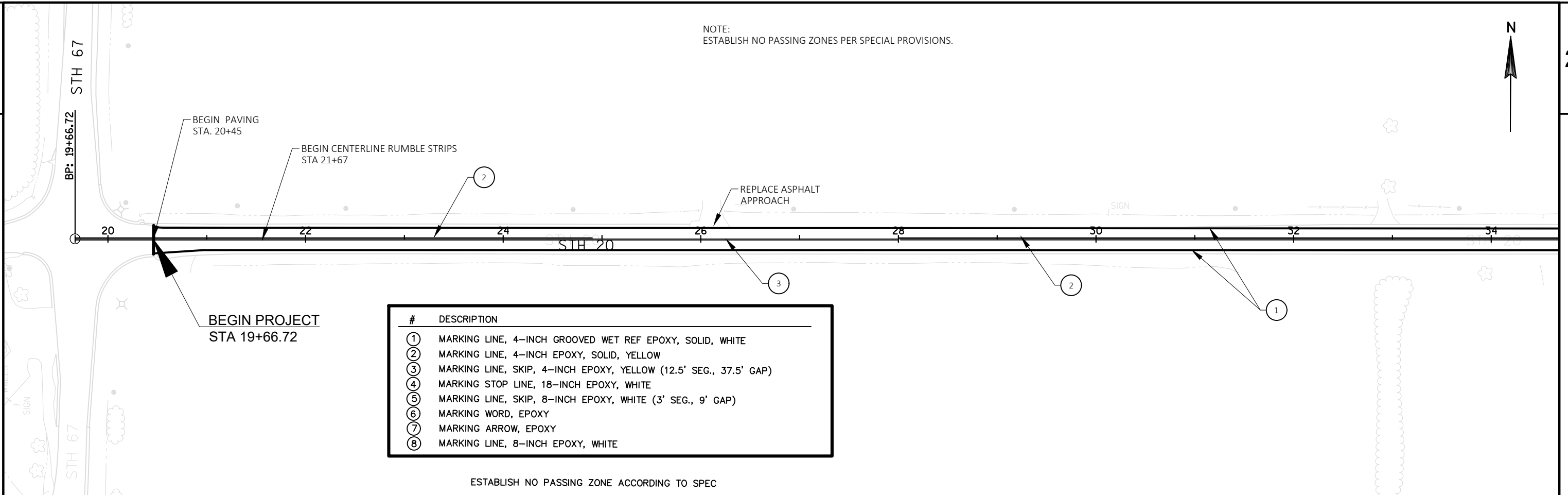


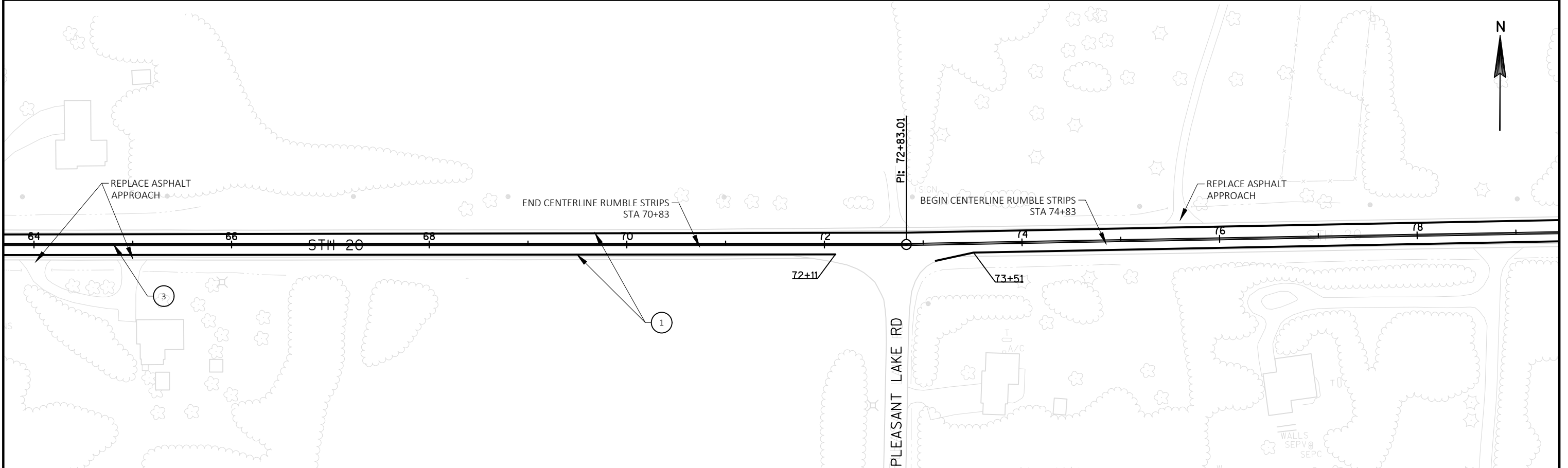
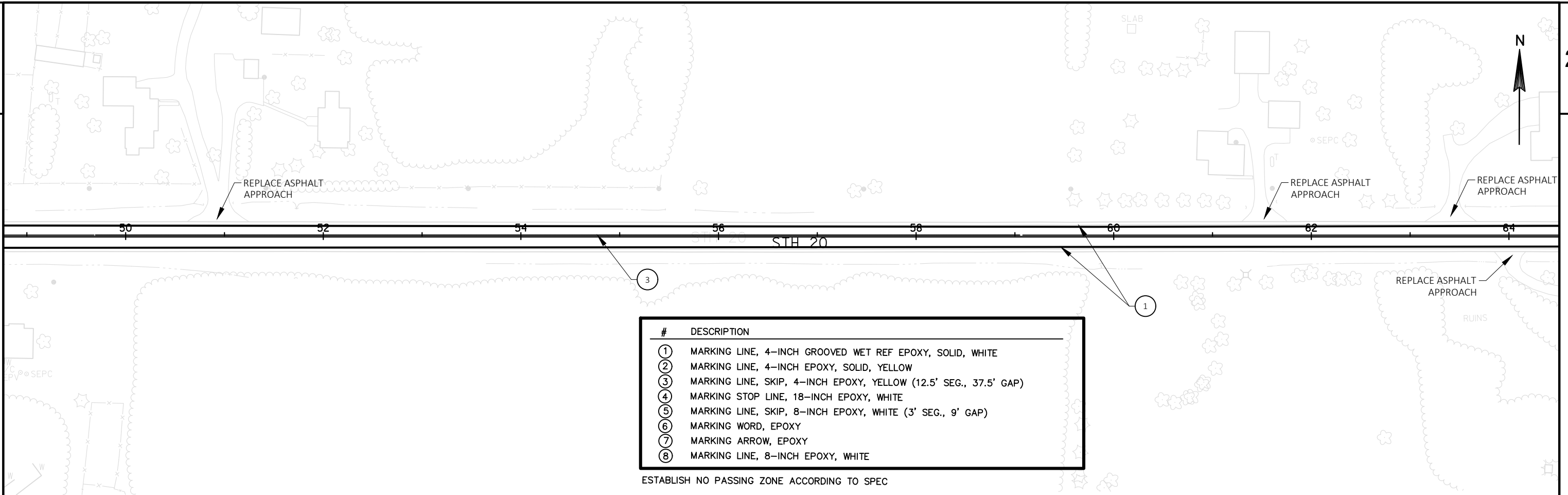
TYPICAL TEMPORARY TRAFFIC CONTROL DETAIL
MOUNTING ON FIXED SUPPORT

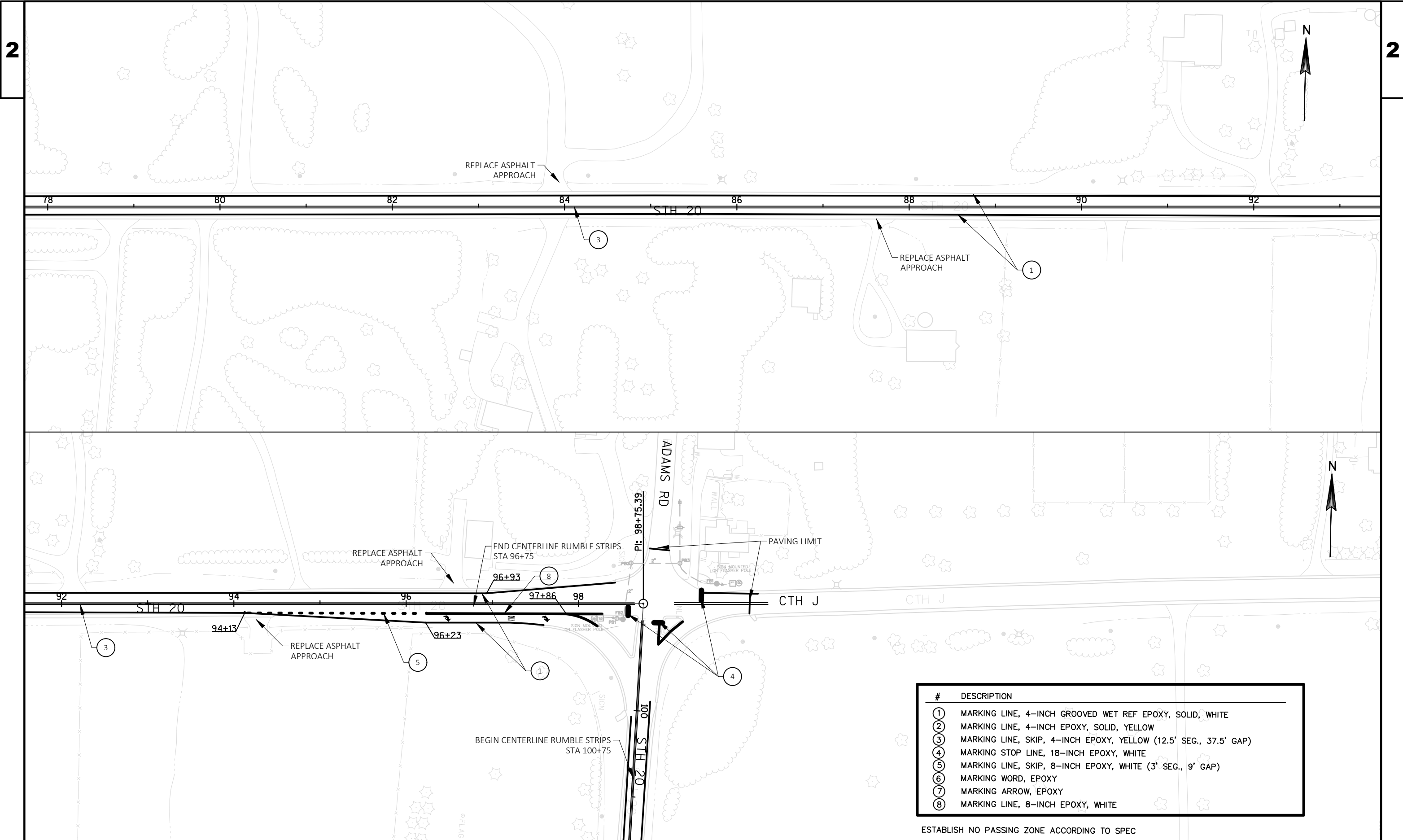
LONG TERM
7 DAYS OR MORE

GENERAL NOTES FOR TRAFFIC CONTROL

1. TRAFFIC CONTROL DRUMS IN TAPERS, SHALL BE EQUIPPED WITH WARNING LIGHTS, TYPE "C", ONE WAY LIGHTS IN TAPERS ONLY, UNLESS OTHERWISE SHOWN.
2. SIGN LOCATIONS ARE APPROXIMATE. THE ACTUAL LOCATION SPACING MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO MEET FIELD CONDITIONS.
3. "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS REFLECTIVE ORANGE.
4. ALL TRAFFIC CONTROL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED IN THE PLANS.
5. BARRICADE STRIPES ARE TO BE SLOPED DOWNWARD IN THE DIRECTION OF TRAFFIC FLOW.







PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PLAN: PLAN DETAILS

SHEET

E

FILE NAME : N:\PDS\C3D\CAD\31300630\021201_PD.DWG
LAYOUT NAME - 024505_PM

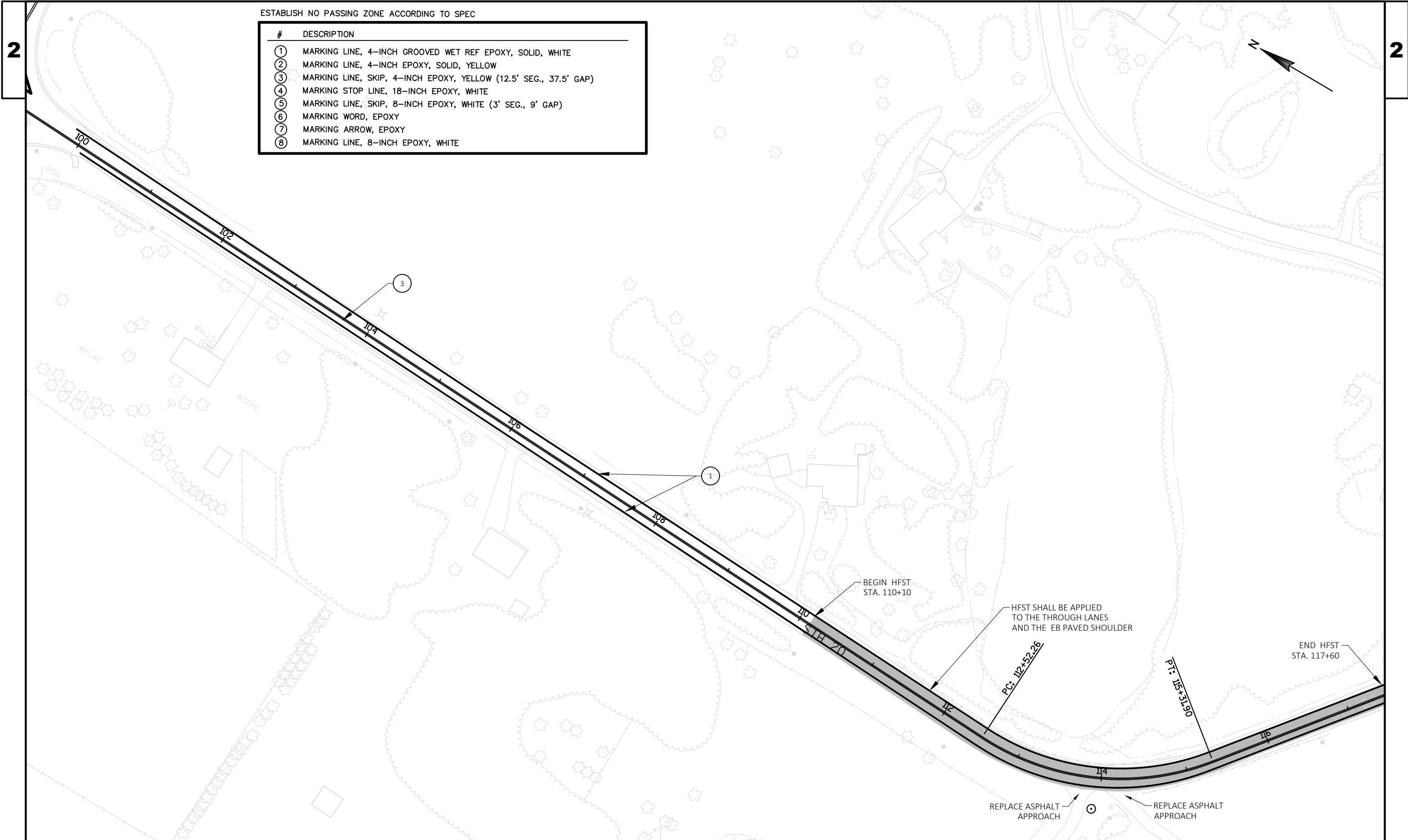
PLOT DATE : 5/1/2019 7:43 AM

PLOT BY : ENGERSON, JONATHAN M

PLOT NAME :

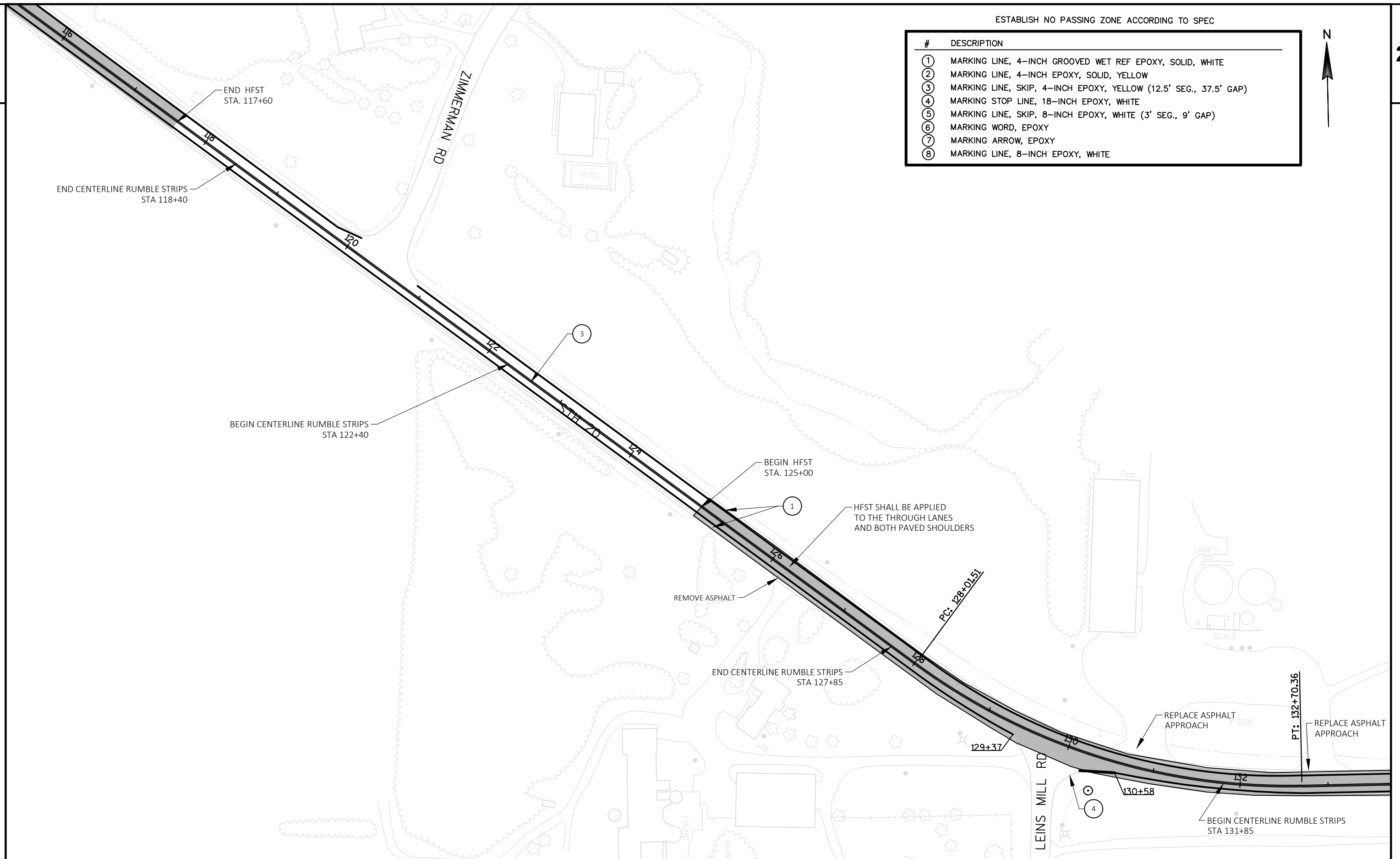
PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42



ESTABLISH NO PASSING ZONE ACCORDING TO SPEC

| # | DESCRIPTION |
|---|------------------------------------------------------------------|
| ① | MARKING LINE, 4-INCH GROOVED WET REF EPOXY, SOLID, WHITE |
| ② | MARKING LINE, 4-INCH EPOXY, SOLID, YELLOW |
| ③ | MARKING LINE, SKIP, 4-INCH EPOXY, YELLOW (12.5' SEG., 37.5' GAP) |
| ④ | MARKING STOP LINE, 18-INCH EPOXY, WHITE |
| ⑤ | MARKING LINE, SKIP, 8-INCH EPOXY, WHITE (3' SEG., 9' GAP) |
| ⑥ | MARKING WORD, EPOXY |
| ⑦ | MARKING ARROW, EPOXY |
| ⑧ | MARKING LINE, 8-INCH EPOXY, WHITE |



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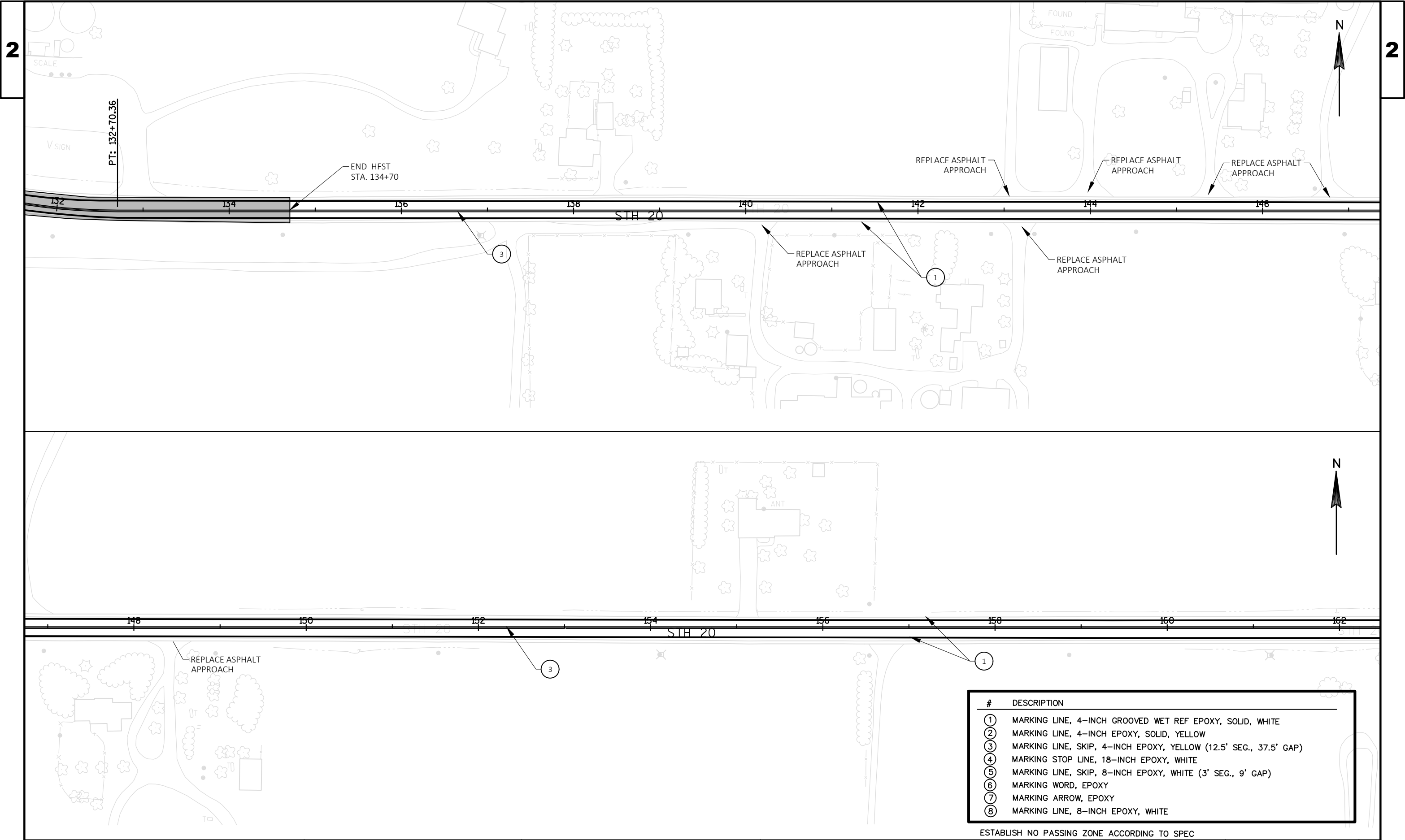
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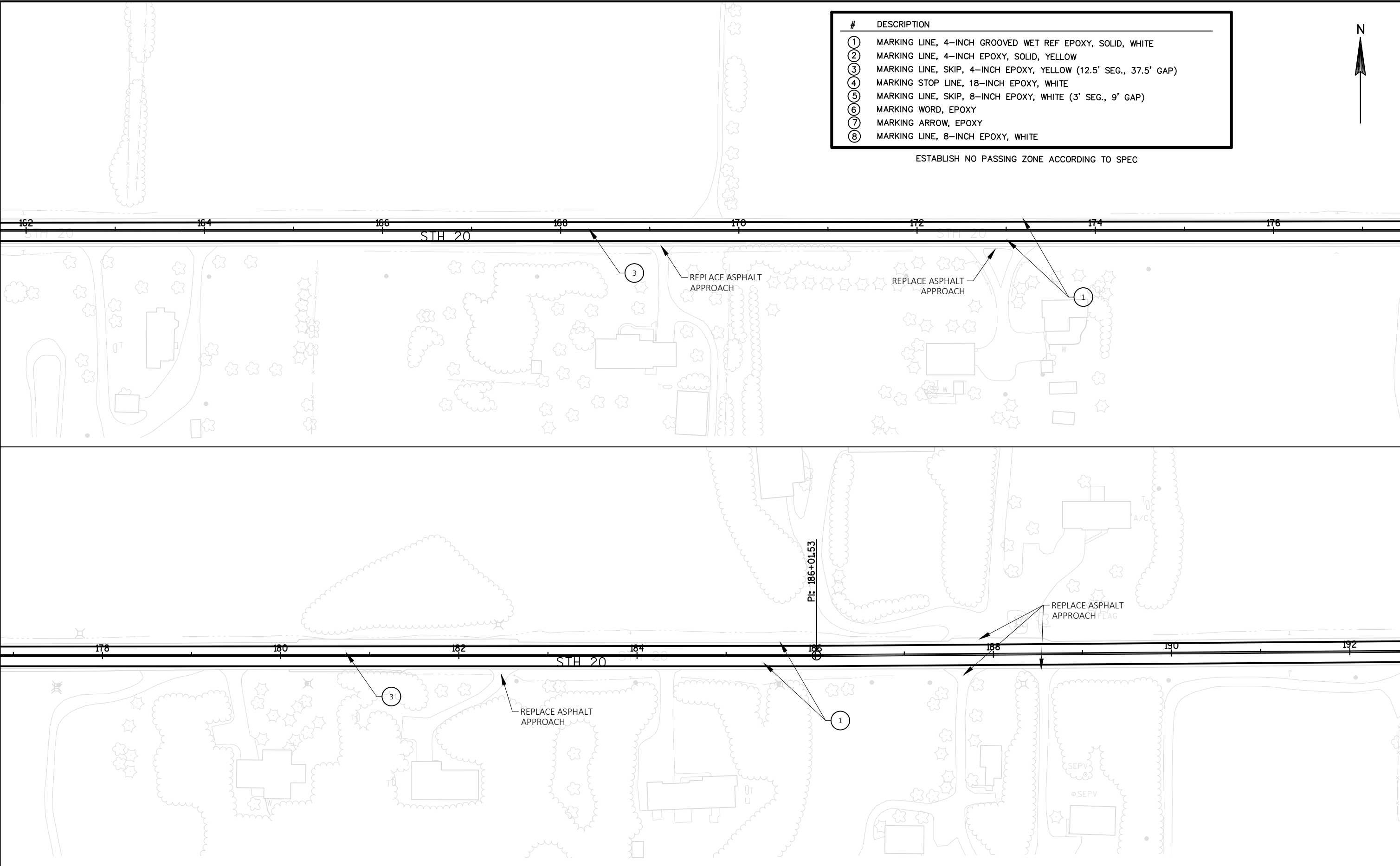
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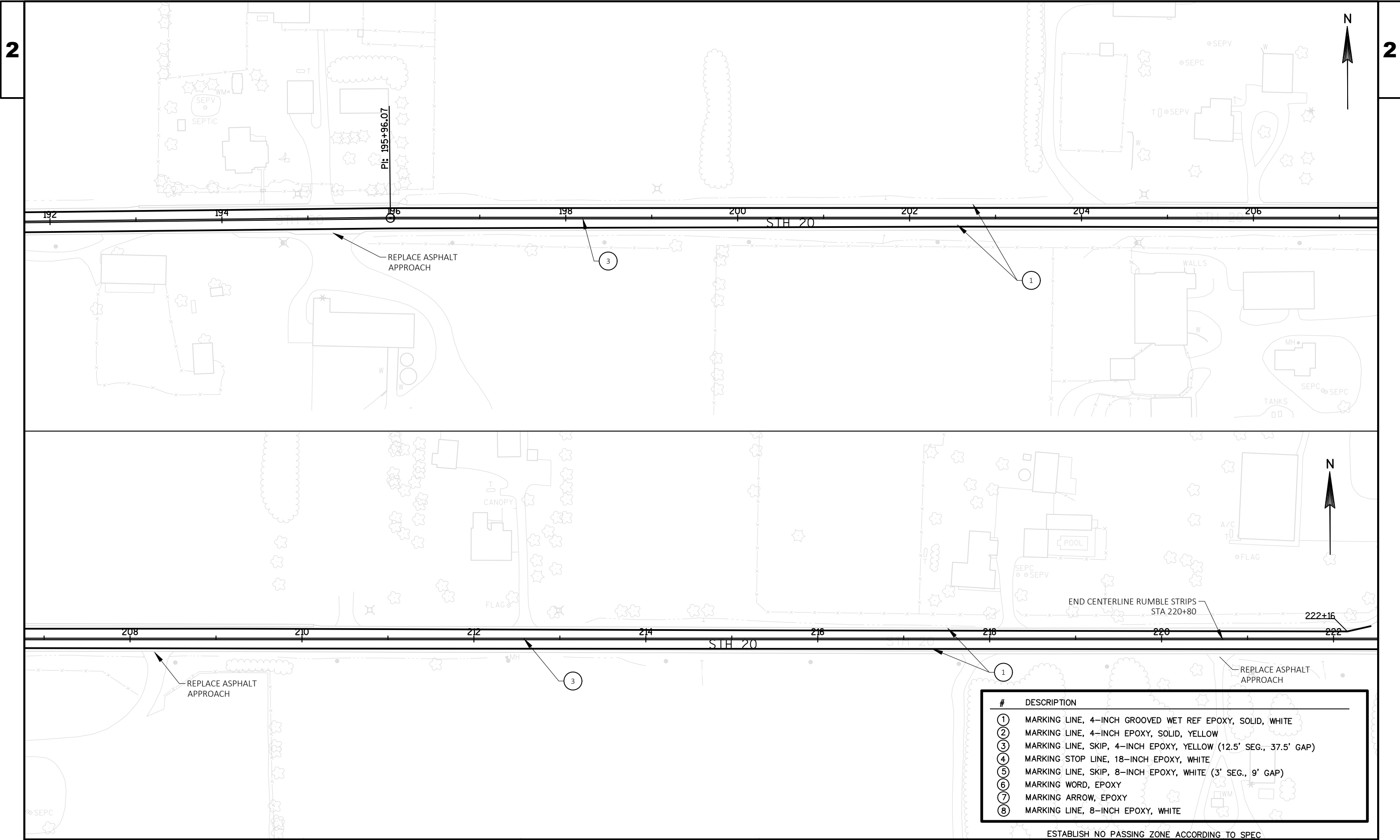
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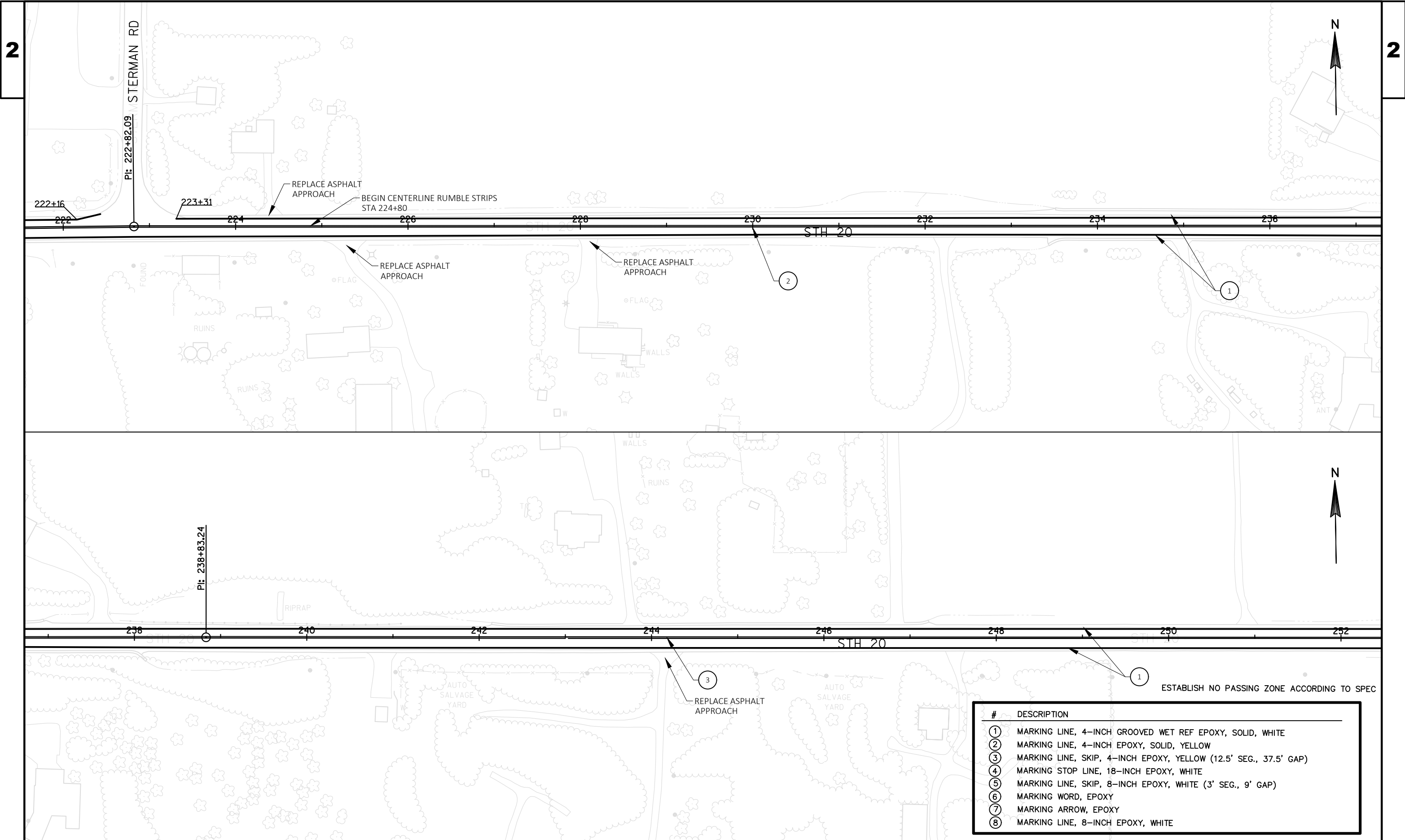
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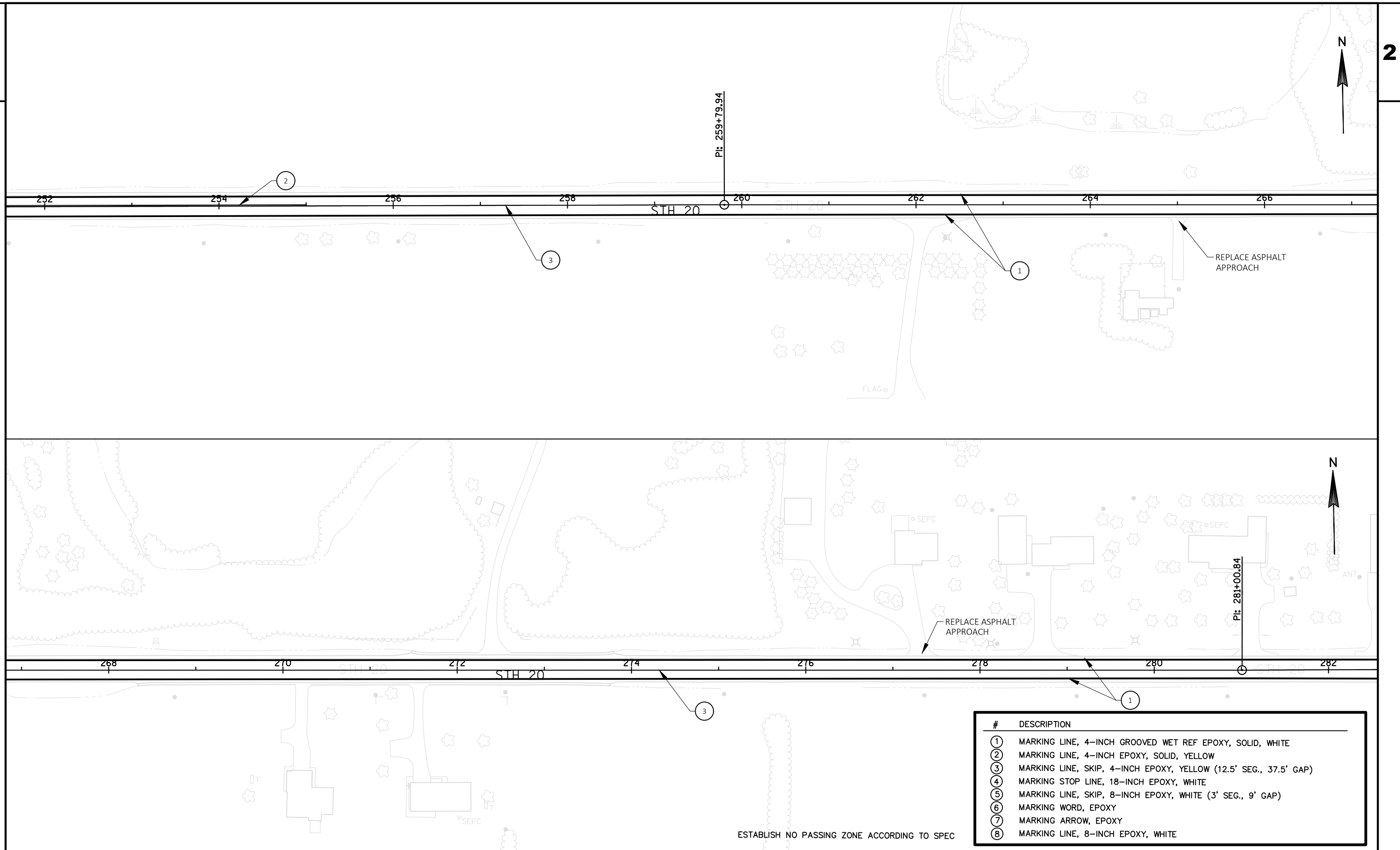
ESTABLISH NO PASSING ZONE ACCORDING TO SPEC





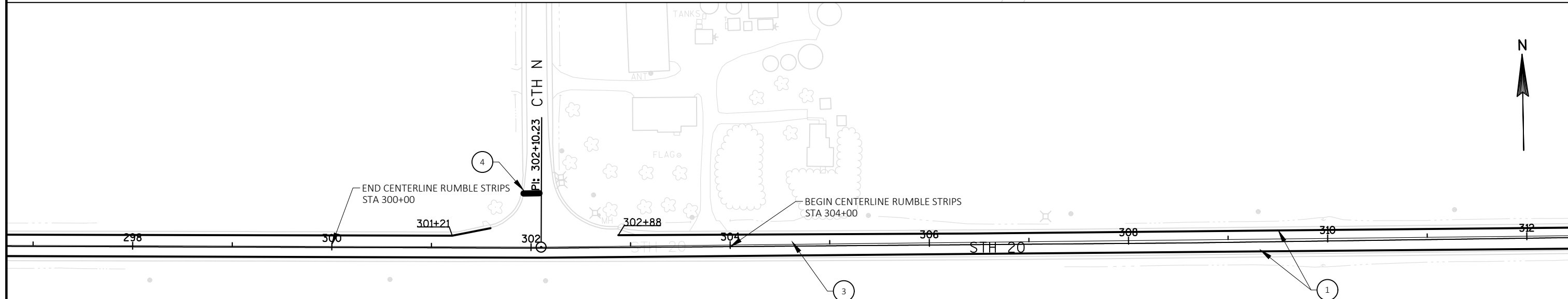
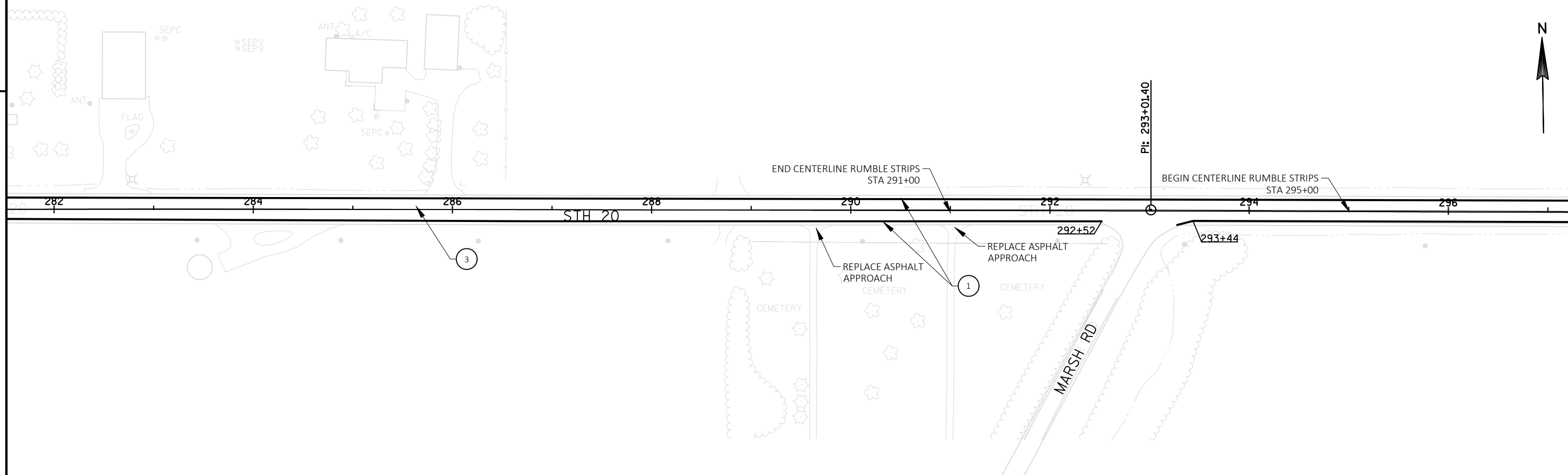


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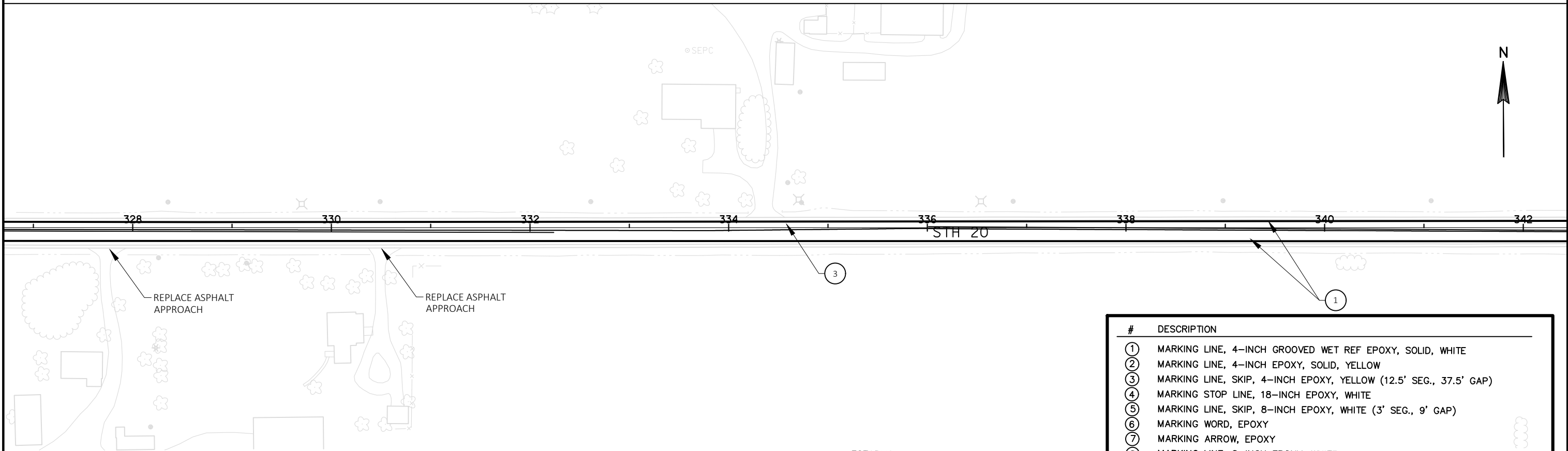
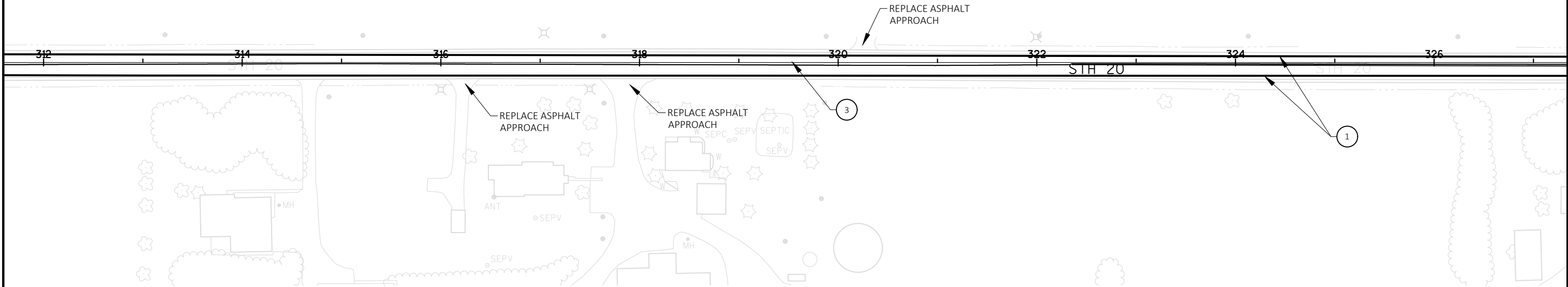
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| | | | | | |
|------------------------|-------------|------------------|-------------------|-------|----------|
| PROJECT NO: 3130-06-60 | HWY: STH 20 | COUNTY: WALWORTH | PLAN: PLAN DETAIL | SHEET | E |
|------------------------|-------------|------------------|-------------------|-------|----------|



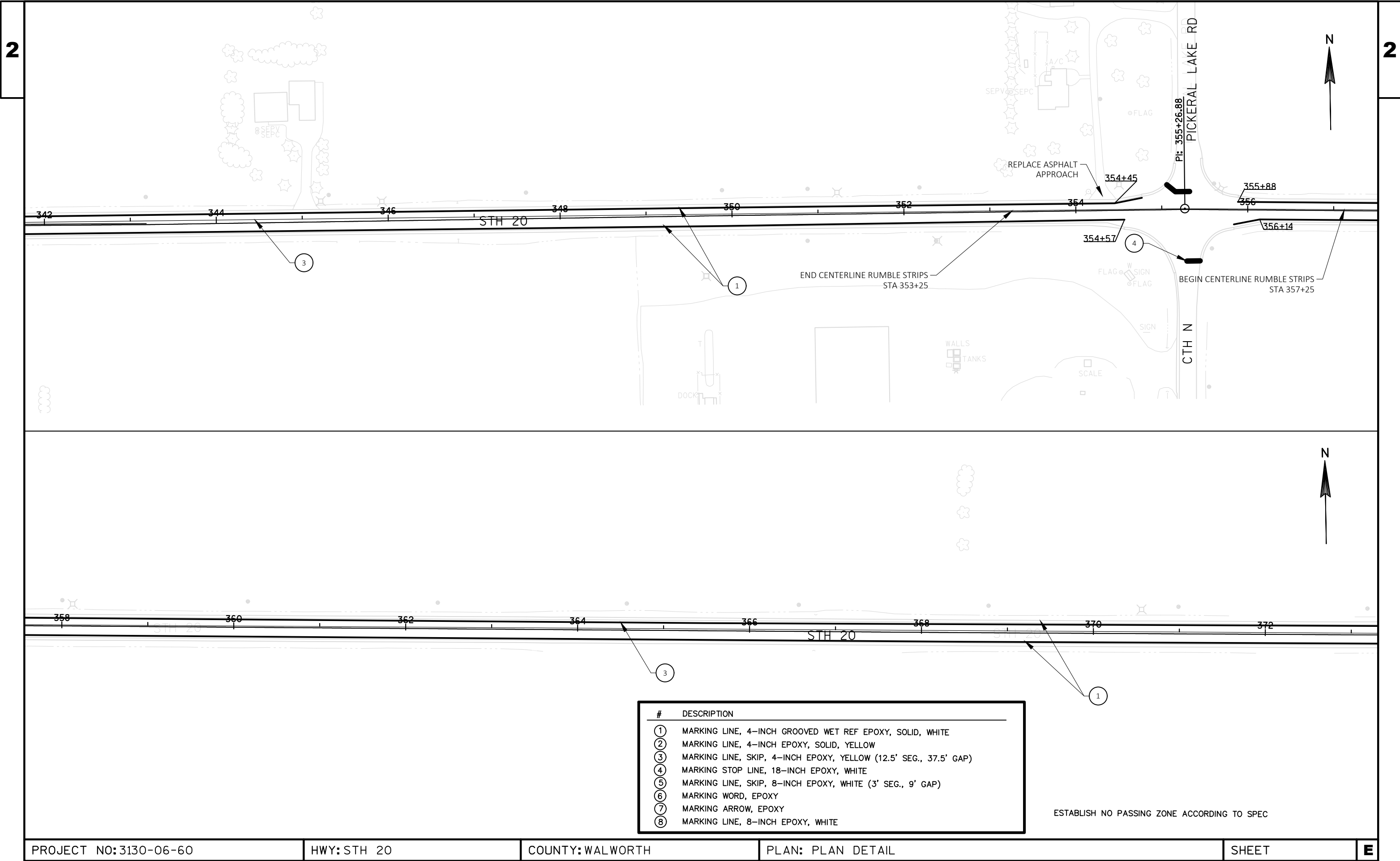
ESTABLISH NO PASSING ZONE ACCORDING TO SPEC

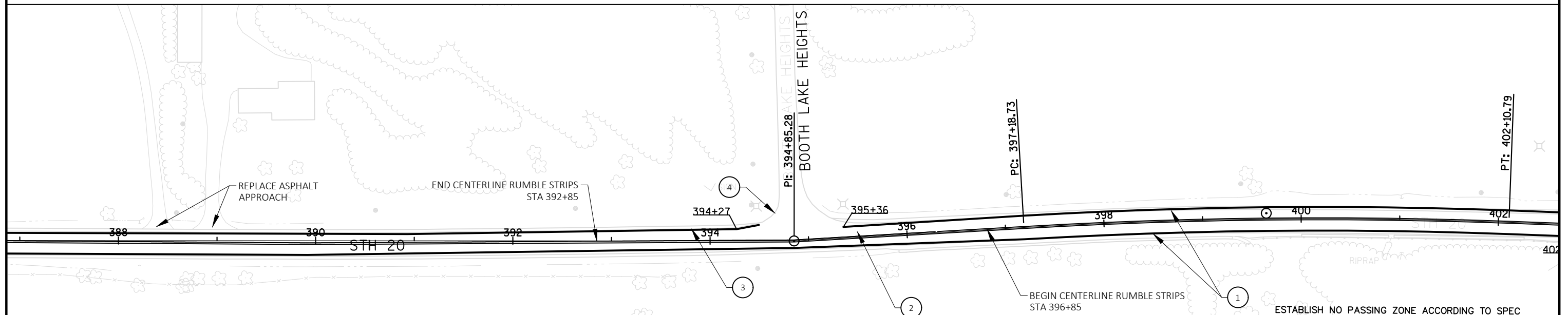
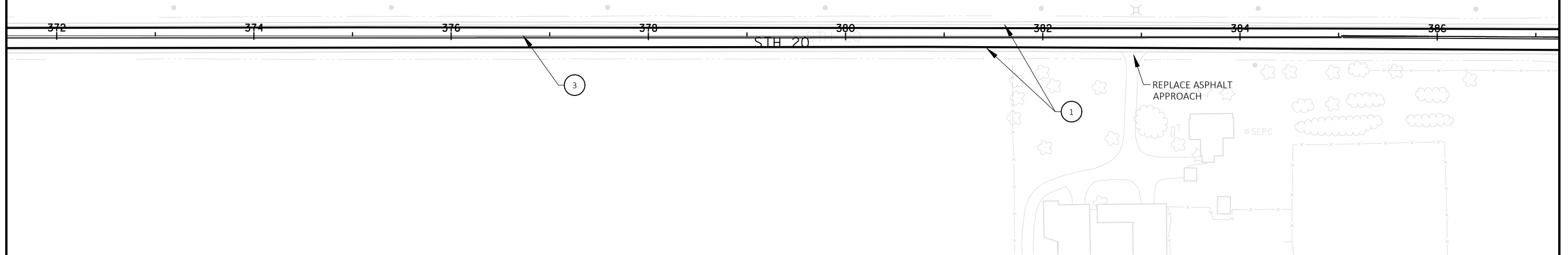
| # | DESCRIPTION |
|---|------------------------------------------------------------------|
| ① | MARKING LINE, 4-INCH GROOVED WET REF EPOXY, SOLID, WHITE |
| ② | MARKING LINE, 4-INCH EPOXY, SOLID, YELLOW |
| ③ | MARKING LINE, SKIP, 4-INCH EPOXY, YELLOW (12.5' SEG., 37.5' GAP) |
| ④ | MARKING STOP LINE, 18-INCH EPOXY, WHITE |
| ⑤ | MARKING LINE, SKIP, 8-INCH EPOXY, WHITE (3' SEG., 9' GAP) |
| ⑥ | MARKING WORD, EPOXY |
| ⑦ | MARKING ARROW, EPOXY |
| ⑧ | MARKING LINE, 8-INCH EPOXY, WHITE |



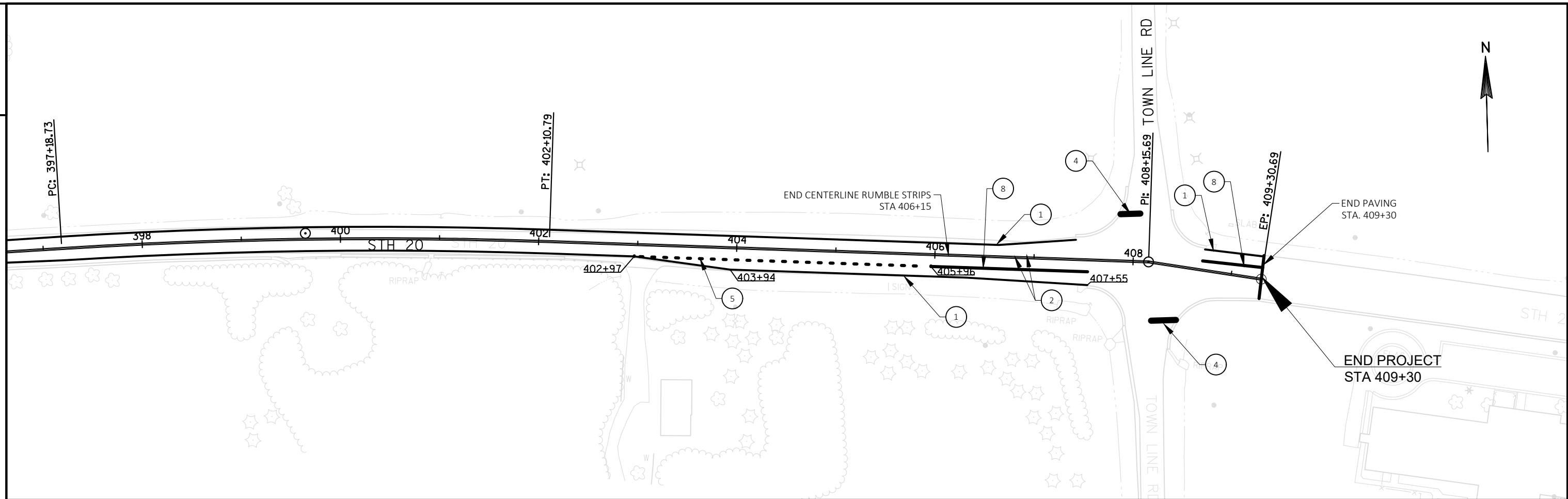
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|---|------------------------------------------------------------------|
| ① | MARKING LINE, 4-INCH GROOVED WET REF EPOXY, SOLID, WHITE |
| ② | MARKING LINE, 4-INCH EPOXY, SOLID, YELLOW |
| ③ | MARKING LINE, SKIP, 4-INCH EPOXY, YELLOW (12.5' SEG., 37.5' GAP) |
| ④ | MARKING STOP LINE, 18-INCH EPOXY, WHITE |
| ⑤ | MARKING LINE, SKIP, 8-INCH EPOXY, WHITE (3' SEG., 9' GAP) |
| ⑥ | MARKING WORD, EPOXY |
| ⑦ | MARKING ARROW, EPOXY |
| ⑧ | MARKING LINE, 8-INCH EPOXY, WHITE |

ESTABLISH NO PASSING ZONE ACCORDING TO SPEC





| # | DESCRIPTION |
|---|------------------------------------------------------------------|
| ① | MARKING LINE, 4-INCH GROOVED WET REF EPOXY, SOLID, WHITE |
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| ⑦ | MARKING ARROW, EPOXY |
| ⑧ | MARKING LINE, 8-INCH EPOXY, WHITE |

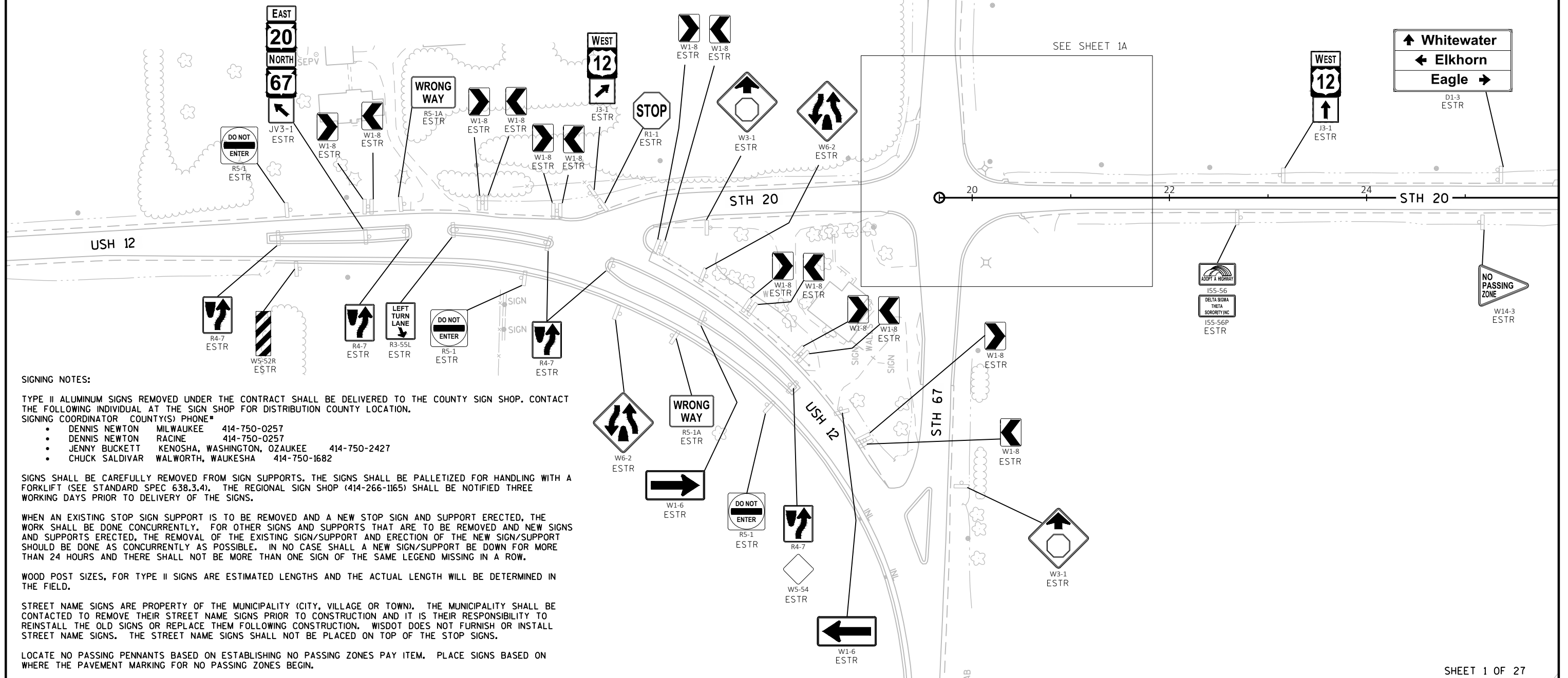


ESTABLISH NO PASSING ZONE ACCORDING TO SPEC

| # | DESCRIPTION |
|---|------------------------------------------------------------------|
| ① | MARKING LINE, 4-INCH GROOVED WET REF EPOXY, SOLID, WHITE |
| ② | MARKING LINE, 4-INCH EPOXY, SOLID, YELLOW |
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| ⑥ | MARKING WORD, EPOXY |
| ⑦ | MARKING ARROW, EPOXY |
| ⑧ | MARKING LINE, 8-INCH EPOXY, WHITE |

LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 PROPOSED SIGN MOUNTED ON POST(S)
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 VARIABLE MESSAGE SIGN
 EXISTING LIGHT OR SIGNAL POLE
 SIGN BRIDGE
 CANTILEVER SIGN BRIDGE
 DENOTES SIGN NUMBER
 INDICATES SIGN SIZE
ESTR EXISTING SIGN TO REMAIN



SIGNING NOTES:

TYPE II ALUMINUM SIGNS REMOVED UNDER THE CONTRACT SHALL BE DELIVERED TO THE COUNTY SIGN SHOP. CONTACT THE FOLLOWING INDIVIDUAL AT THE SIGN SHOP FOR DISTRIBUTION COUNTY LOCATION.

SIGNING COORDINATOR COUNTY(S) PHONE*

- DENNIS NEWTON MILWAUKEE 414-750-0257
- DENNIS NEWTON RACINE 414-750-0257
- JENNY BUCKETT KENOSHA, WASHINGTON, OZAUKEE 414-750-2427
- CHUCK SALDIVAR WALWORTH, WAUKESHA 414-750-1682

SIGNS SHALL BE CAREFULLY REMOVED FROM SIGN SUPPORTS. THE SIGNS SHALL BE PALLETIZED FOR HANDLING WITH A FORKLIFT (SEE STANDARD SPEC 638.3.4). THE REGIONAL SIGN SHOP (414-266-1165) SHALL BE NOTIFIED THREE WORKING DAYS PRIOR TO DELIVERY OF THE SIGNS.

WHEN AN EXISTING STOP SIGN SUPPORT IS TO BE REMOVED AND A NEW STOP SIGN AND SUPPORT ERECTED, THE WORK SHALL BE DONE CONCURRENTLY. FOR OTHER SIGNS AND SUPPORTS THAT ARE TO BE REMOVED AND NEW SIGNS AND SUPPORTS ERECTED, THE REMOVAL OF THE EXISTING SIGN/SUPPORT AND ERECTION OF THE NEW SIGN/SUPPORT SHOULD BE DONE AS CONCURRENTLY AS POSSIBLE. IN NO CASE SHALL A NEW SIGN/SUPPORT BE DOWN FOR MORE THAN 24 HOURS AND THERE SHALL NOT BE MORE THAN ONE SIGN OF THE SAME LEGEND MISSING IN A ROW.

WOOD POST SIZES, FOR TYPE II SIGNS ARE ESTIMATED LENGTHS AND THE ACTUAL LENGTH WILL BE DETERMINED IN THE FIELD.

STREET NAME SIGNS ARE PROPERTY OF THE MUNICIPALITY (CITY, VILLAGE OR TOWN). THE MUNICIPALITY SHALL BE CONTACTED TO REMOVE THEIR STREET NAME SIGNS PRIOR TO CONSTRUCTION AND IT IS THEIR RESPONSIBILITY TO REINSTALL THE OLD SIGNS OR REPLACE THEM FOLLOWING CONSTRUCTION. WISDOT DOES NOT FURNISH OR INSTALL STREET NAME SIGNS. THE STREET NAME SIGNS SHALL NOT BE PLACED ON TOP OF THE STOP SIGNS.

LOCATE NO PASSING PENNANTS BASED ON ESTABLISHING NO PASSING ZONES PAY ITEM. PLACE SIGNS BASED ON WHERE THE PAVEMENT MARKING FOR NO PASSING ZONES BEGIN.

SHEET 1 OF 27

PROJECT NO: 3130-06-60

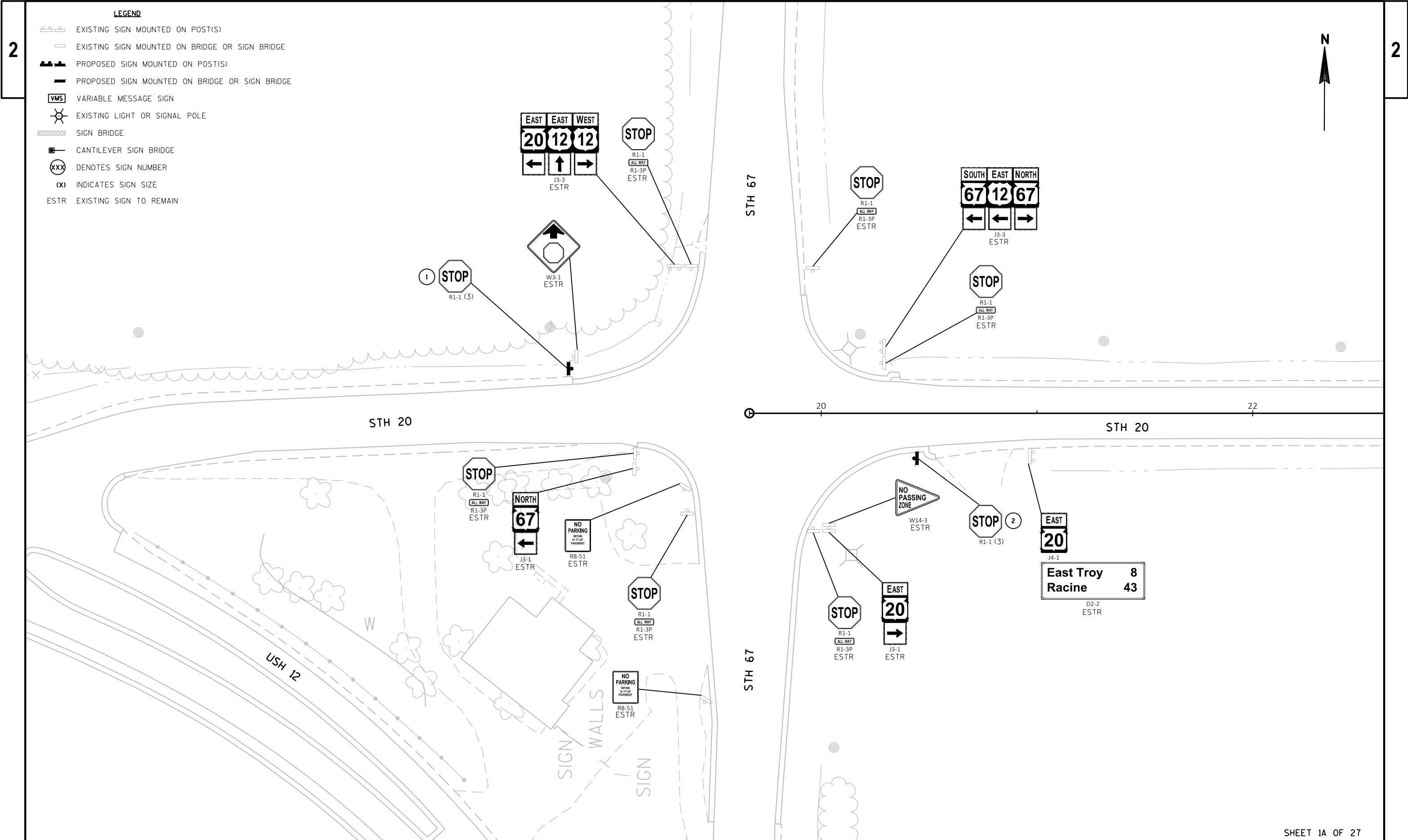
HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

SHEET

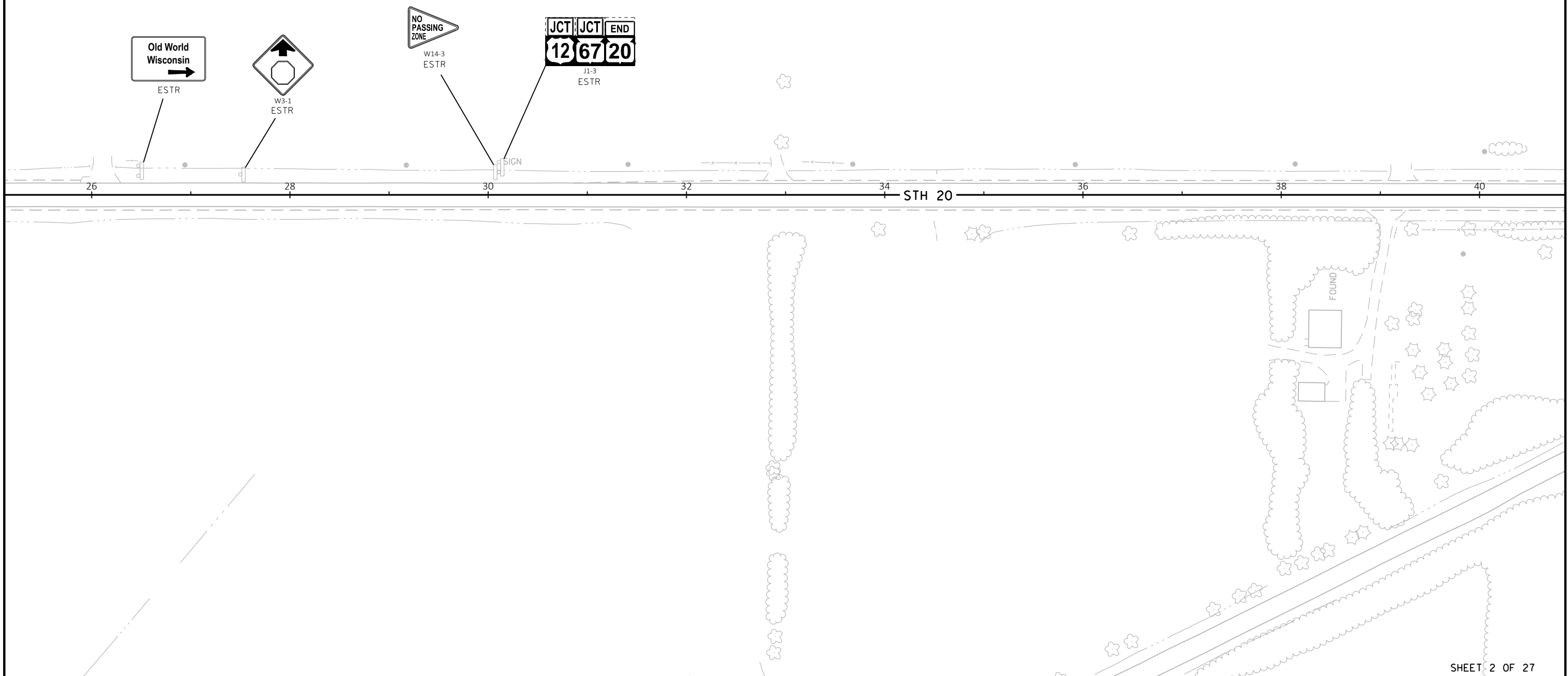
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




LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- PROPOSED SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- VMS VARIABLE MESSAGE SIGN
- EXISTING LIGHT OR SIGNAL POLE
- SIGN BRIDGE
- CANTILEVER SIGN BRIDGE
- DENOTES SIGN NUMBER
- (X) INDICATES SIGN SIZE
- ESTR EXISTING SIGN TO REMAIN

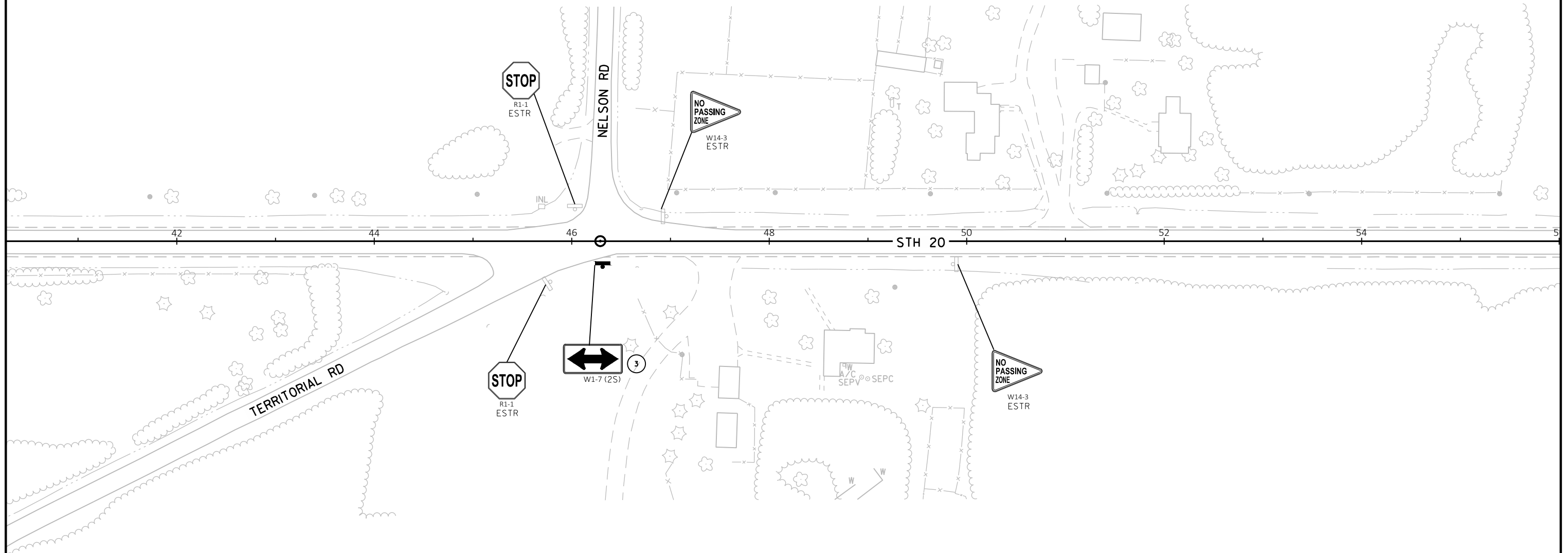
NOTE:
LOCATE NO PASSING PENNANTS BASED ON ESTABLISHING NO PASSING ZONES
PAY ITEM. PLACE SIGNS BASED ON WHERE THE PAVEMENT MARKING FOR
NO PASSING ZONES BEGIN.



LEGEND

-  EXISTING SIGN MOUNTED ON POST(S)
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 PROPOSED SIGN MOUNTED ON POST(S)
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 VARIABLE MESSAGE SIGN
 EXISTING LIGHT OR SIGNAL POLE
 SIGN BRIDGE
 CANTILEVER SIGN BRIDGE
 DENOTES SIGN NUMBER
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SHEET 3 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

SHEET

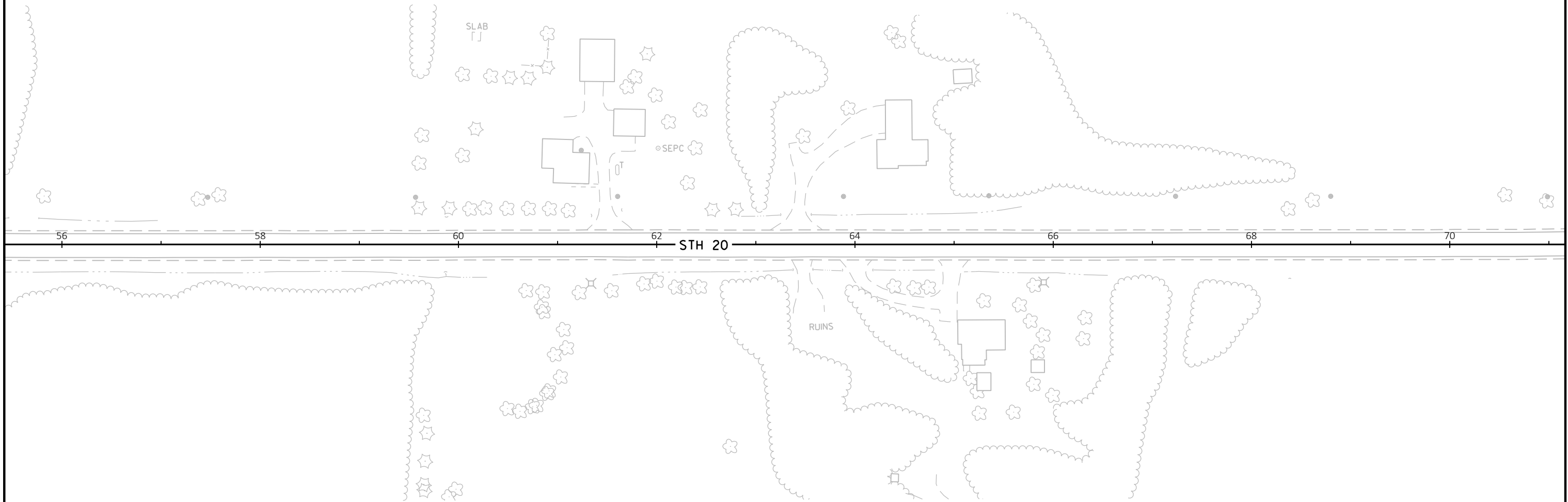
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LEGEND

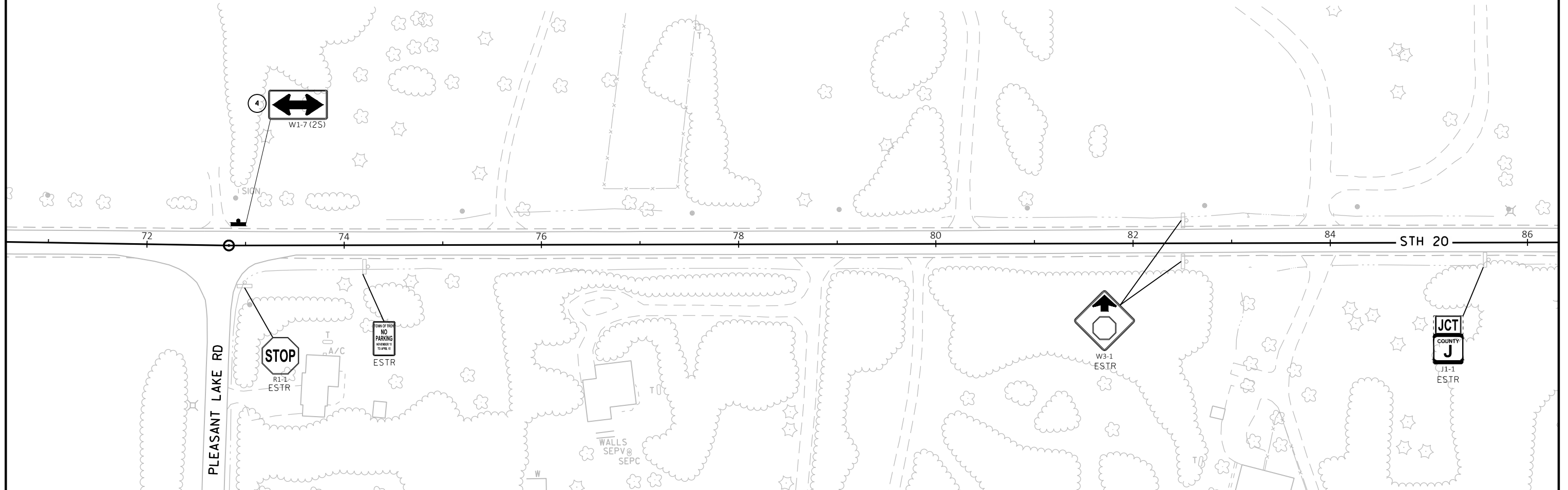
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- EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
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- PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- VARIABLE MESSAGE SIGN
- EXISTING LIGHT OR SIGNAL POLE
- SIGN BRIDGE
- CANTILEVER SIGN BRIDGE
- DENOTES SIGN NUMBER
- INDICATES SIGN SIZE
- ESTR EXISTING SIGN TO REMAIN

NO SIGNS THIS SHEET



**LEGEND**

- EXISTING SIGN MOUNTED ON POST(S)
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 PROPOSED SIGN MOUNTED ON POST(S)
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 VARIABLE MESSAGE SIGN
 EXISTING LIGHT OR SIGNAL POLE
 SIGN BRIDGE
 CANTILEVER SIGN BRIDGE
 DENOTES SIGN NUMBER
 INDICATES SIGN SIZE
ESTR EXISTING SIGN TO REMAIN



SHEET 5 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20











COUNTY: WALWORTH

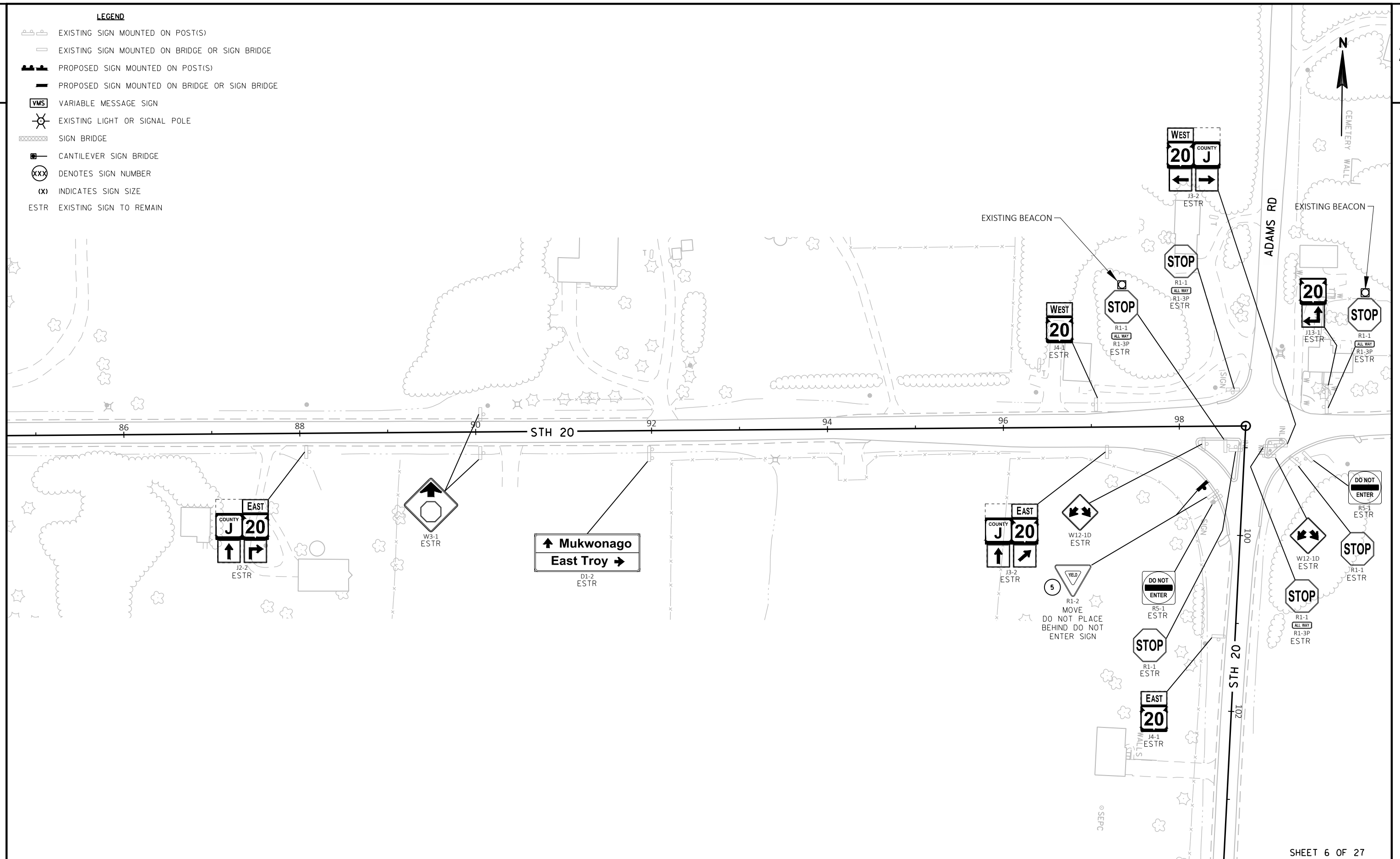
PERMANENT SIGNING

SHEET

E

LEGEND

-  EXISTING SIGN MOUNTED ON POST(S)
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 PROPOSED SIGN MOUNTED ON POST(S)
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 VARIABLE MESSAGE SIGN
 EXISTING LIGHT OR SIGNAL POLE
 SIGN BRIDGE
 CANTILEVER SIGN BRIDGE
 DENOTES SIGN NUMBER
 INDICATES SIGN SIZE
ESTR EXISTING SIGN TO REMAIN



PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

SHEET

E

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LAYOUT NAME - SHEET-06

PLOT DATE : 1/25/2019 1:37 PM











PLOT BY : MARTENS, KAREN L

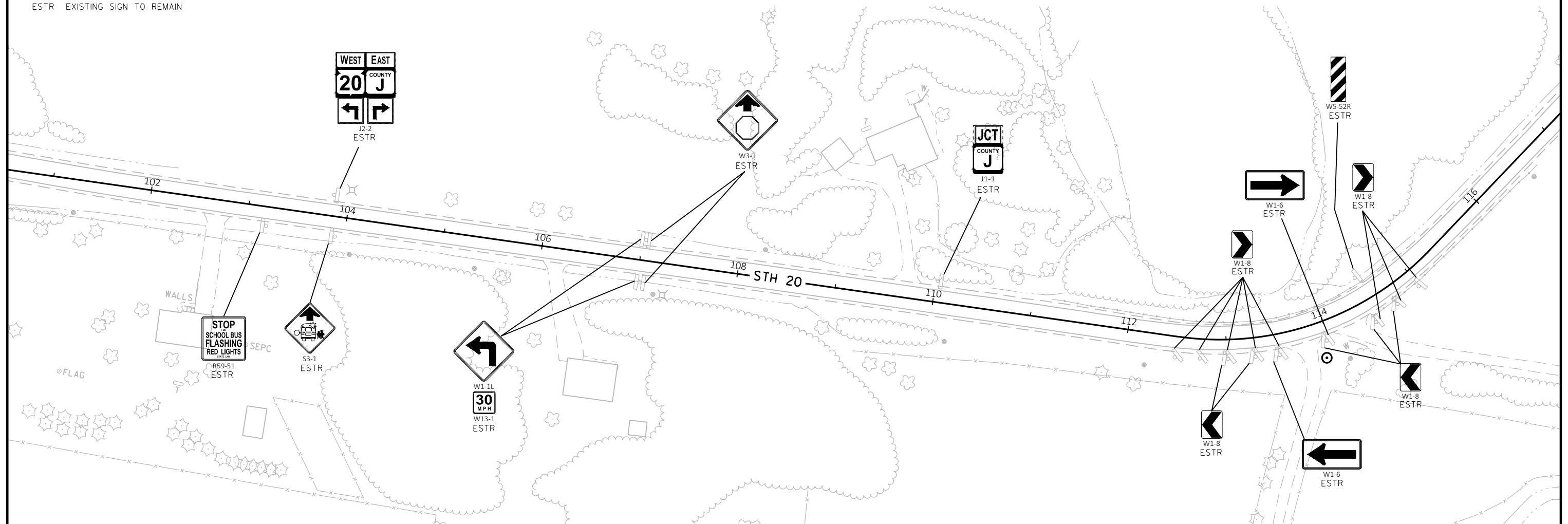
PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADDs SHEET 42

LEGEND

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 PROPOSED SIGN MOUNTED ON POST(S)
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 VARIABLE MESSAGE SIGN
 EXISTING LIGHT OR SIGNAL POLE
 SIGN BRIDGE
 CANTILEVER SIGN BRIDGE
 DENOTES SIGN NUMBER
 INDICATES SIGN SIZE
ESTR EXISTING SIGN TO REMAIN



SHEET 7 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

SHEET

E

FILE NAME : N:\PDS\C3D\CAD\31300630\SIGN\023201_PS.DWG
LAYOUT NAME - SHEET-07

PLOT DATE : 1/25/2019 1:37 PM

PLOT BY : MARTENS, KAREN L

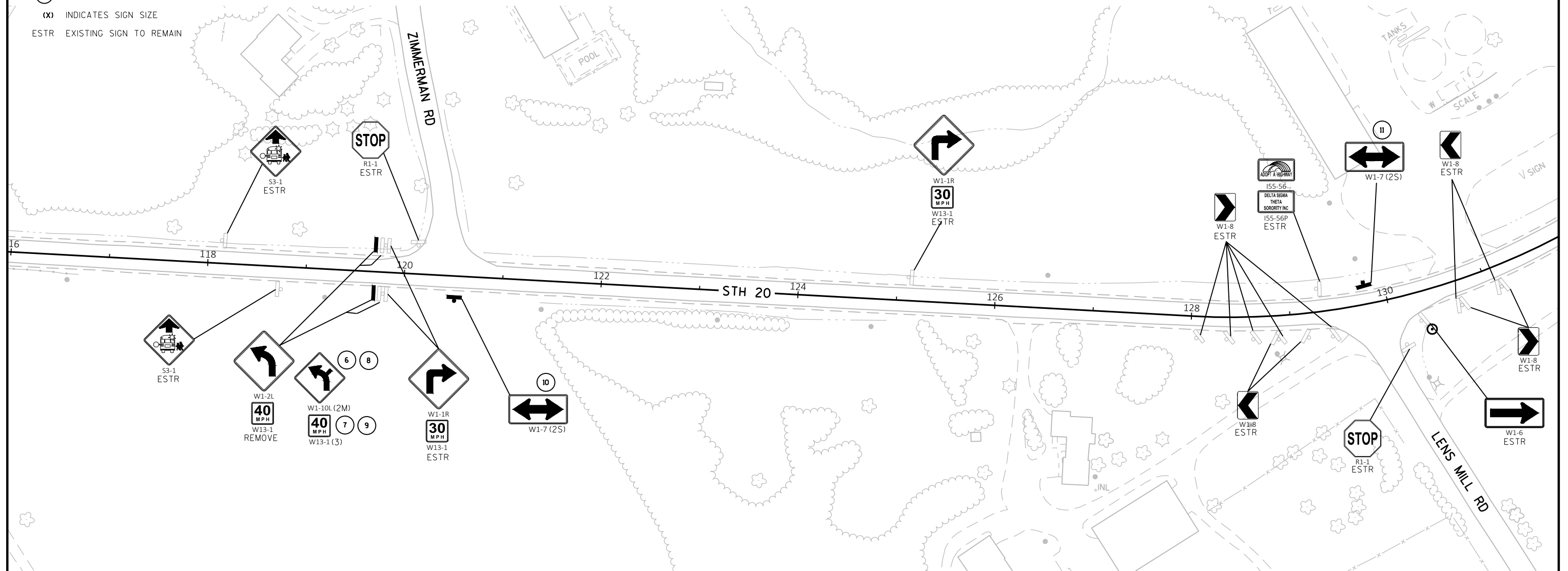
PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADDs SHEET 42

LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
PROPOSED SIGN MOUNTED ON POST(S)
PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
VMS VARIABLE MESSAGE SIGN
EXISTING LIGHT OR SIGNAL POLE
SIGN BRIDGE
CANTILEVER SIGN BRIDGE
DENOTES SIGN NUMBER
(X) INDICATES SIGN SIZE
ESTR EXISTING SIGN TO REMAIN



SHEET 8 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH











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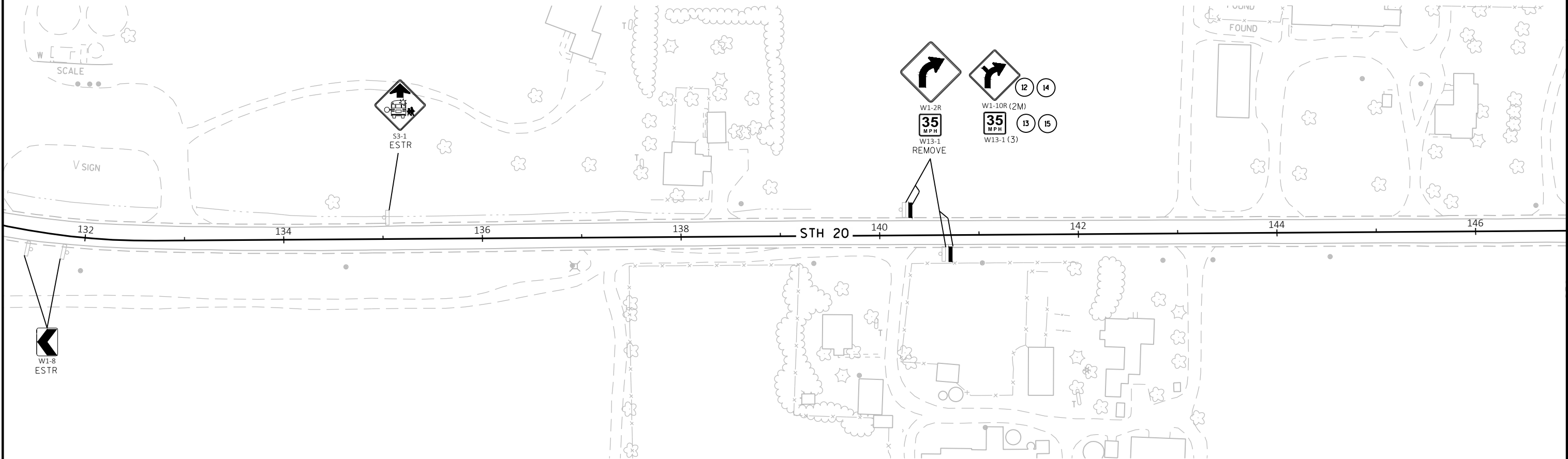
SHEET

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LEGEND

-  EXISTING SIGN MOUNTED ON POST(S)
-  EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
-  PROPOSED SIGN MOUNTED ON POST(S)
-  PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
-  VARIABLE MESSAGE SIGN
-  EXISTING LIGHT OR SIGNAL POLE
-  SIGN BRIDGE
-  CANTILEVER SIGN BRIDGE
-  DENOTES SIGN NUMBER
-  INDICATES SIGN SIZE
- ESTR EXISTING SIGN TO REMAIN

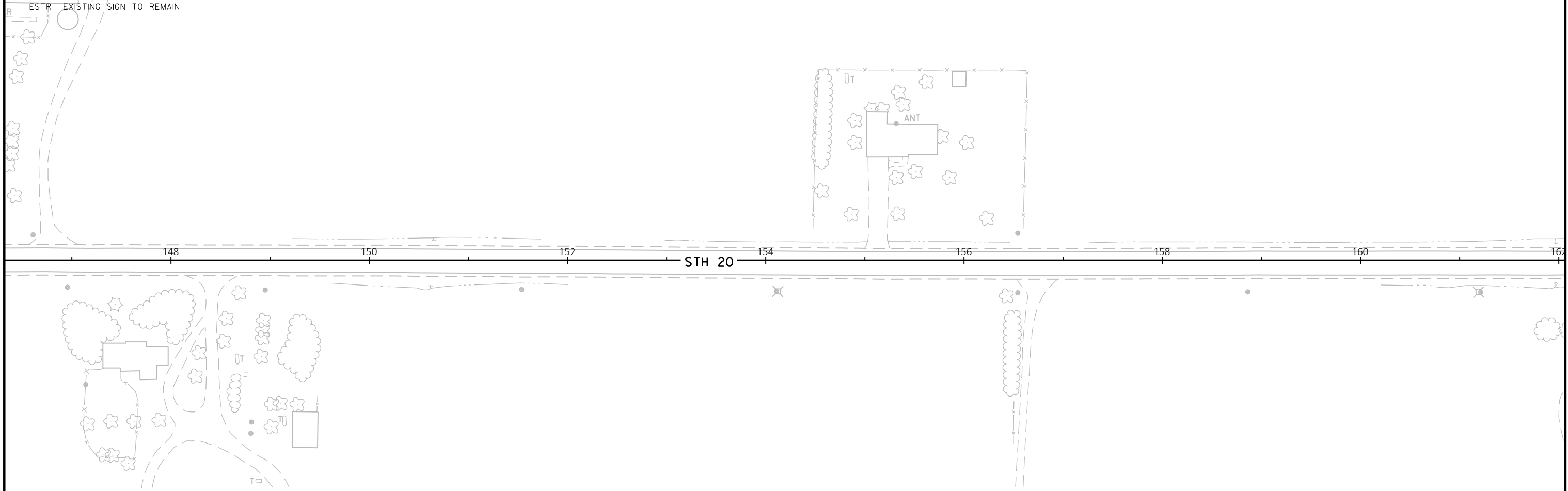




LEGEND











- EXISTING SIGN MOUNTED ON POST(S)
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- PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- VARIABLE MESSAGE SIGN
- EXISTING LIGHT OR SIGNAL POLE
- SIGN BRIDGE
- CANTILEVER SIGN BRIDGE
- DENOTES SIGN NUMBER
- INDICATES SIGN SIZE

NO SIGNS THIS SHEET

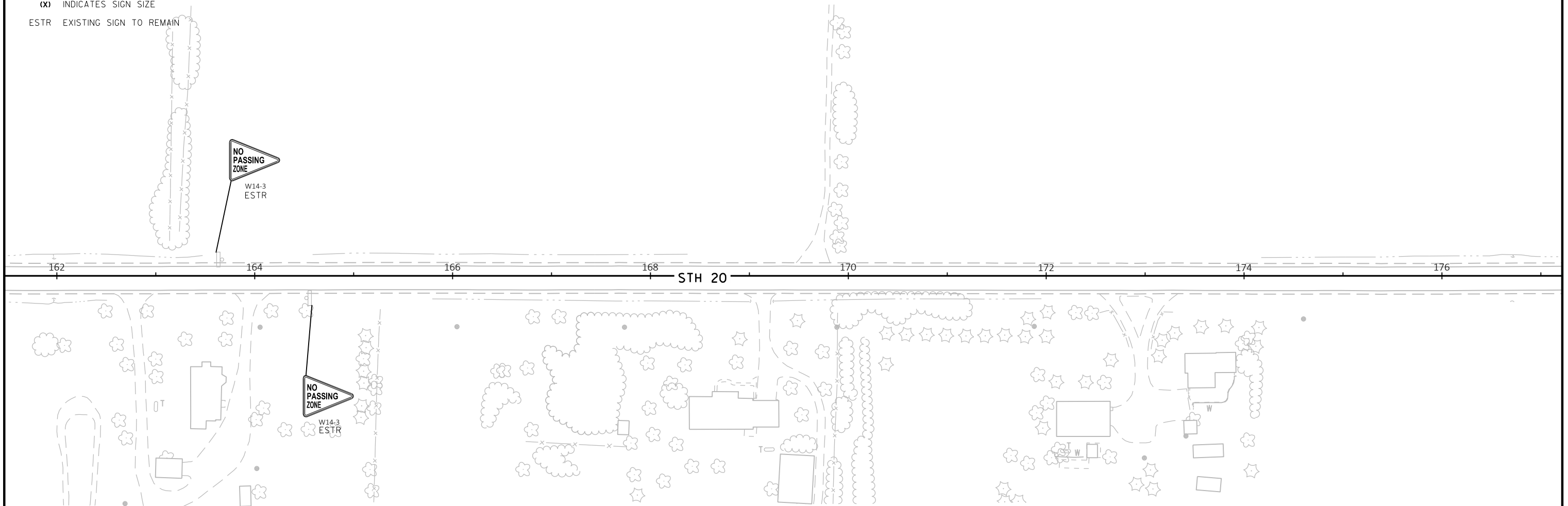




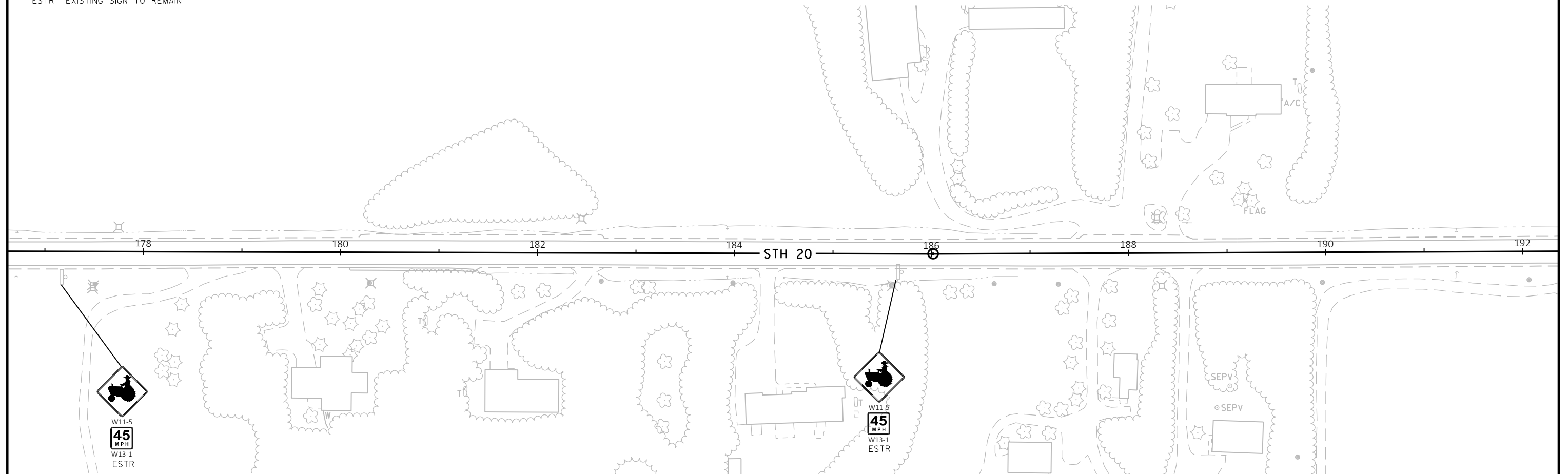
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-  PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
-  VARIABLE MESSAGE SIGN
-  EXISTING LIGHT OR SIGNAL POLE
-  SIGN BRIDGE
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NOTE:
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PAY ITEM. PLACE SIGNS BASED ON WHERE THE PAVEMENT MARKING FOR
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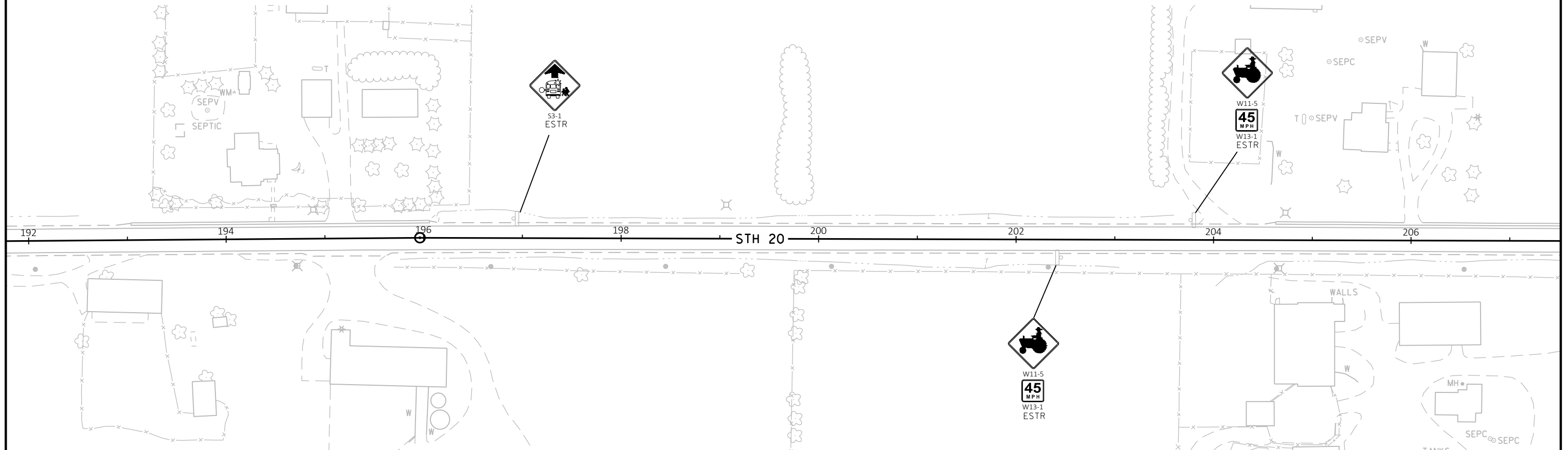


| | |
|-----------------------------------------------------------------------------------|------------------------------------------------|
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|  | EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | PROPOSED SIGN MOUNTED ON POST(S) |
|  | PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE |
|  | VARIABLE MESSAGE SIGN |
|  | EXISTING LIGHT OR SIGNAL POLE |
|  | SIGN BRIDGE |
|  | CANTILEVER SIGN BRIDGE |
|  | DENOTES SIGN NUMBER |
|  | INDICATES SIGN SIZE |
| ESTR | EXISTING SIGN TO REMAIN |



**LEGEND**

- EXISTING SIGN MOUNTED ON POST(S)
 EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
 PROPOSED SIGN MOUNTED ON POST(S)
 PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
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 SIGN BRIDGE
 CANTILEVER SIGN BRIDGE
 DENOTES SIGN NUMBER
 INDICATES SIGN SIZE
ESTR EXISTING SIGN TO REMAIN



SHEET 13 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

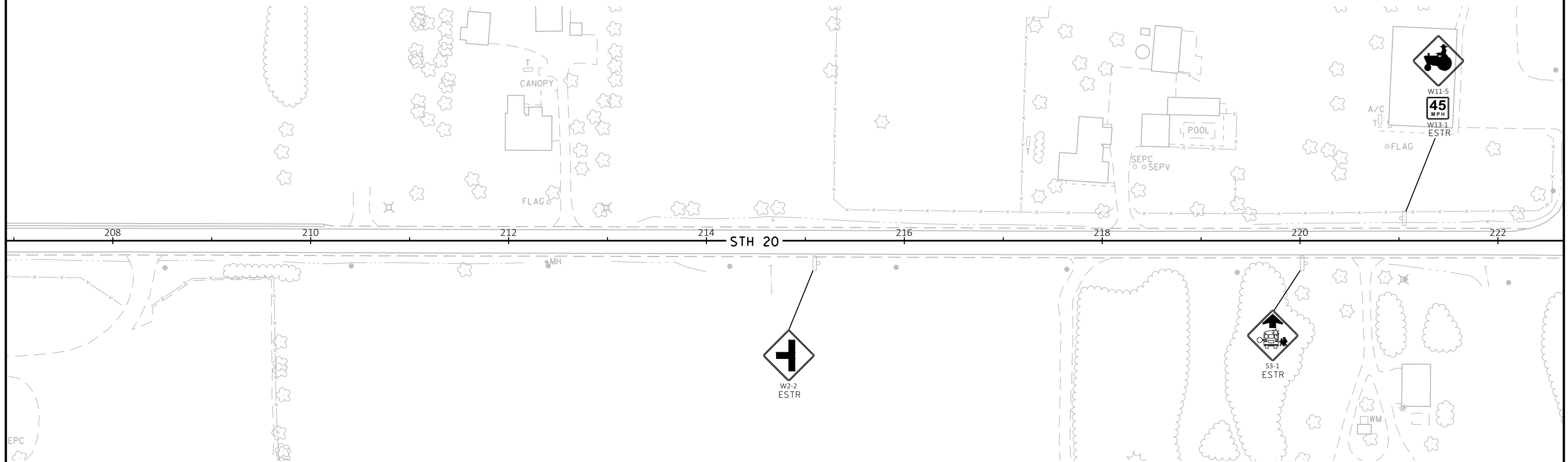
SHEET

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LEGEND

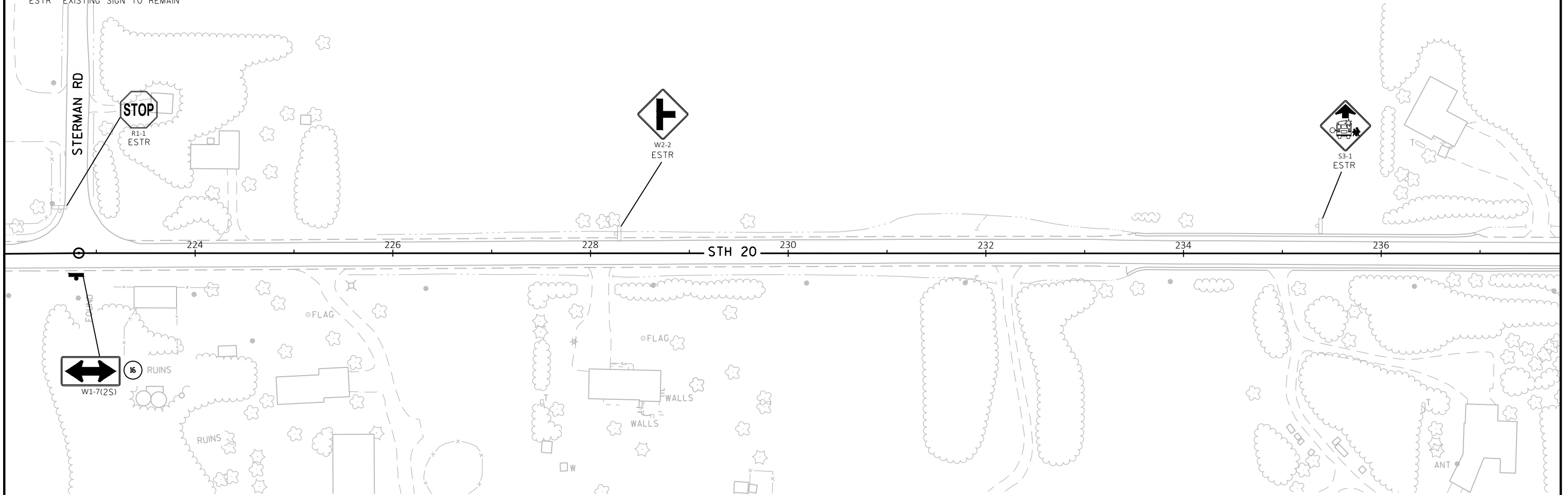
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- PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- VARIABLE MESSAGE SIGN
- EXISTING LIGHT OR SIGNAL POLE
- SIGN BRIDGE
- CANTILEVER SIGN BRIDGE
- DENOTES SIGN NUMBER
- INDICATES SIGN SIZE
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LEGEND

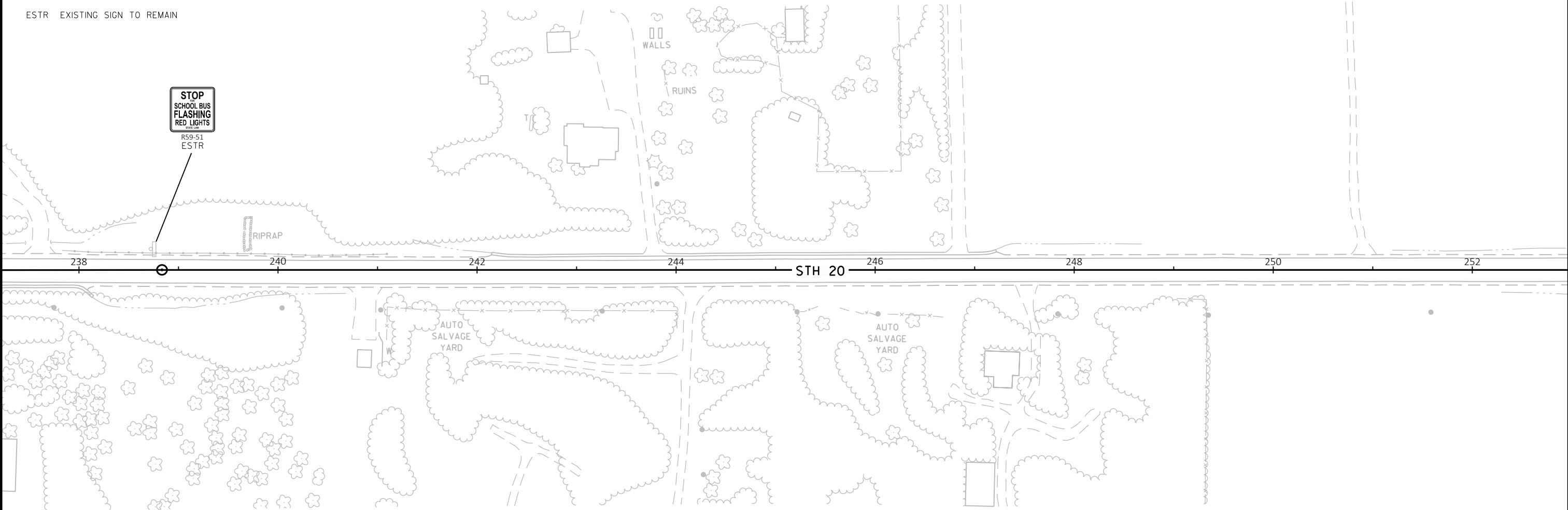
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- PROPOSED SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
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- SIGN BRIDGE
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










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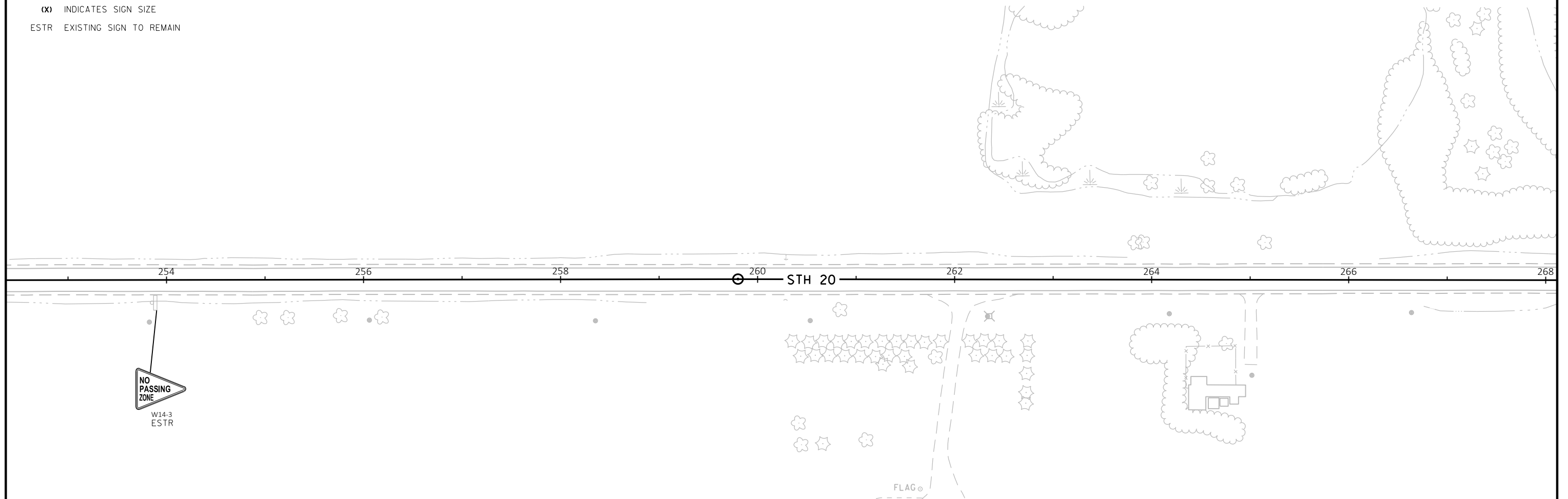
- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- PROPOSED SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- VARIABLE MESSAGE SIGN
- EXISTING LIGHT OR SIGNAL POLE
- SIGN BRIDGE
- CANTILEVER SIGN BRIDGE
- DENOTES SIGN NUMBER
- INDICATES SIGN SIZE
- ESTR EXISTING SIGN TO REMAIN



LEGEND

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-  EXISTING SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
-  PROPOSED SIGN MOUNTED ON POST(S)
-  PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
-  VARIABLE MESSAGE SIGN
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SHEET 17 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

SHEET

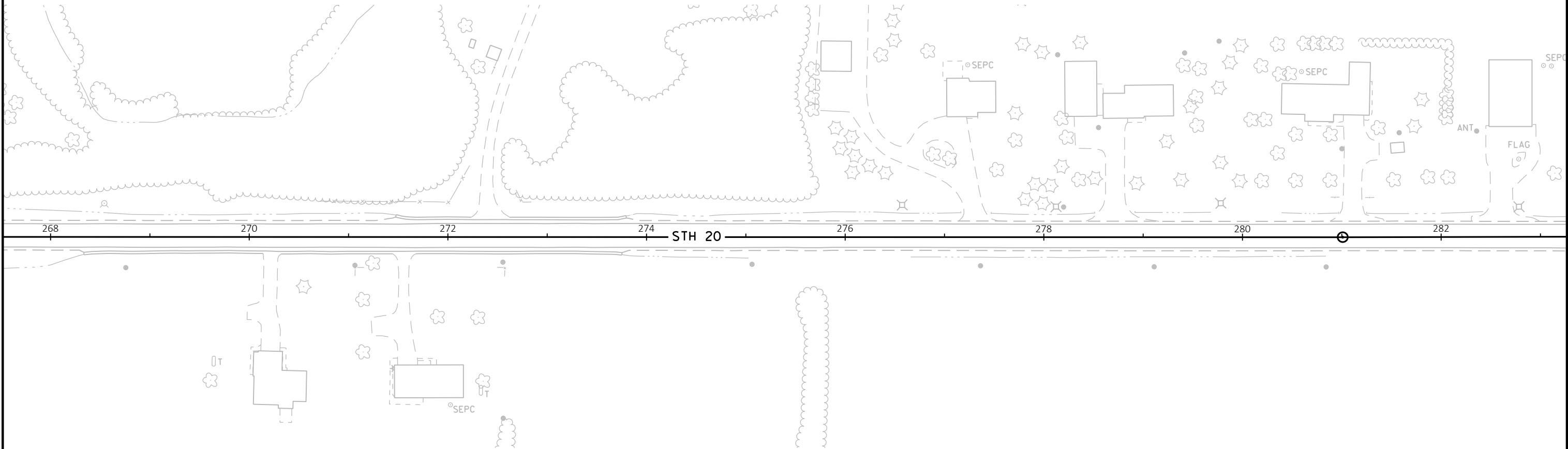
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LEGEND











- EXISTING SIGN MOUNTED ON POST(S)
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- PROPOSED SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON BRIDGE OR SIGN BRIDGE
- VARIABLE MESSAGE SIGN
- EXISTING LIGHT OR SIGNAL POLE
- SIGN BRIDGE
- CANTILEVER SIGN BRIDGE
- DENOTES SIGN NUMBER
- INDICATES SIGN SIZE
- ESTR EXISTING SIGN TO REMAIN

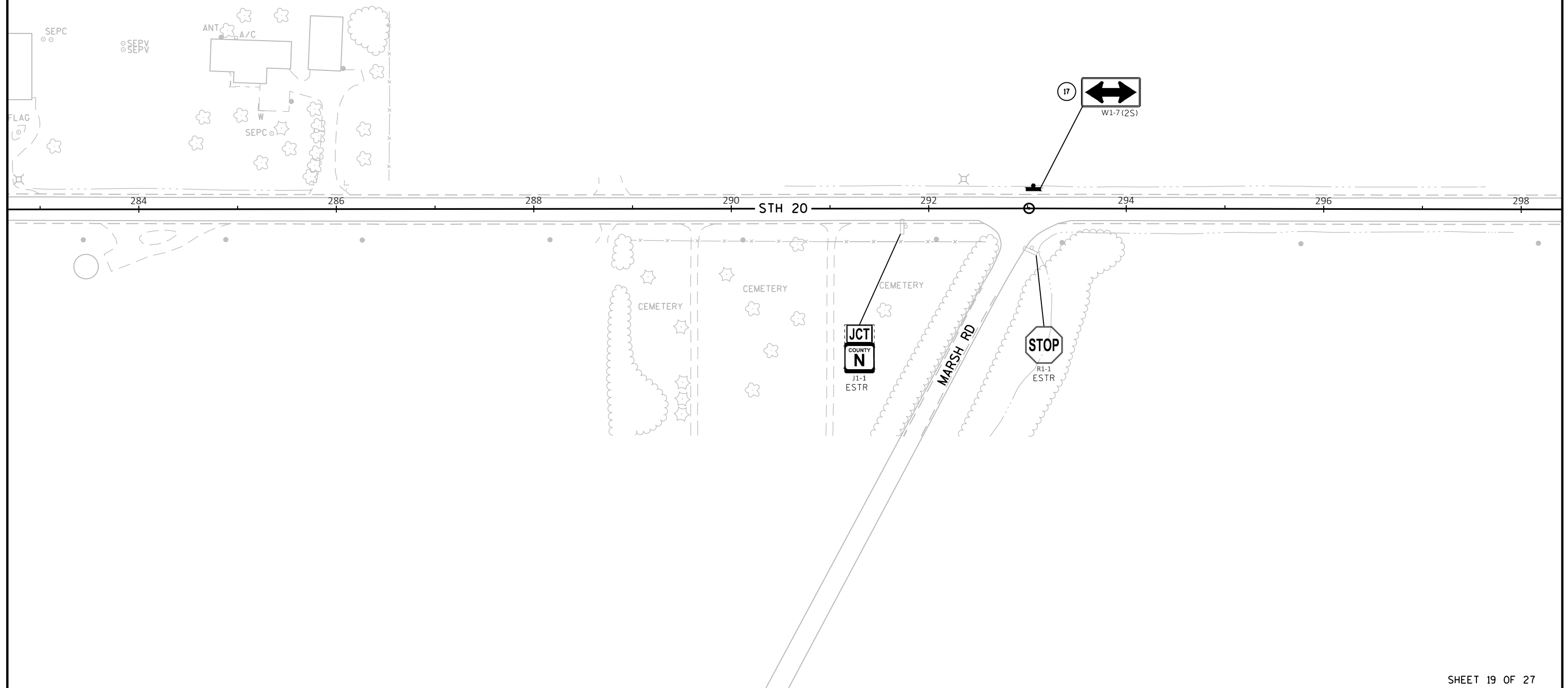
NO SIGNS THIS SHEET





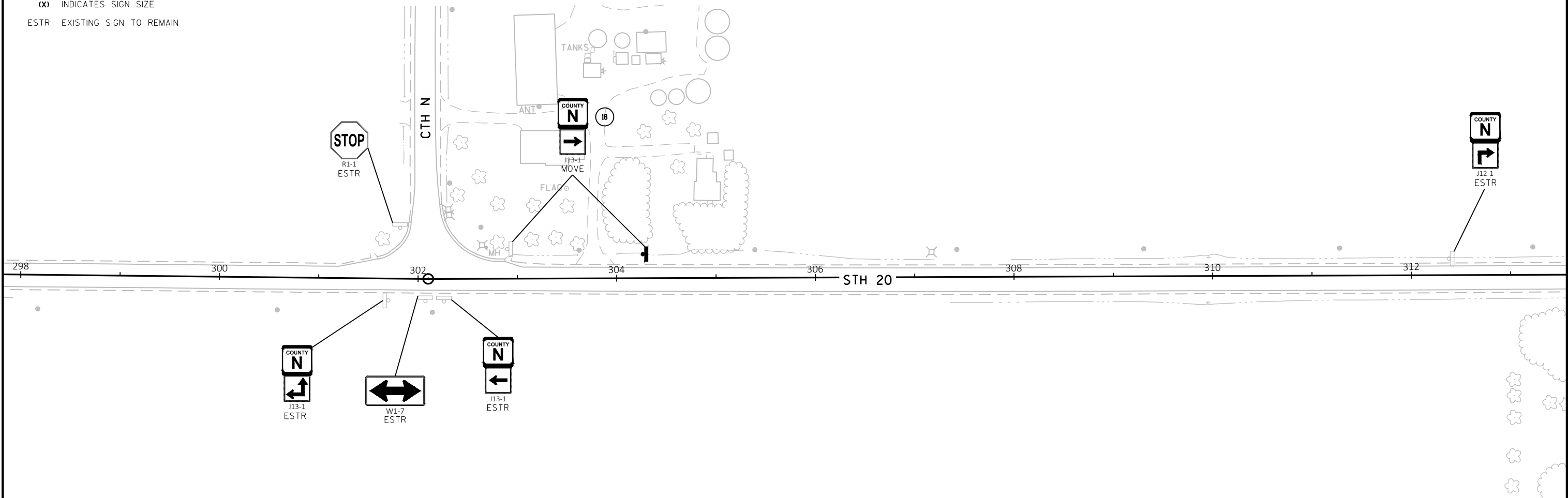
LEGEND

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












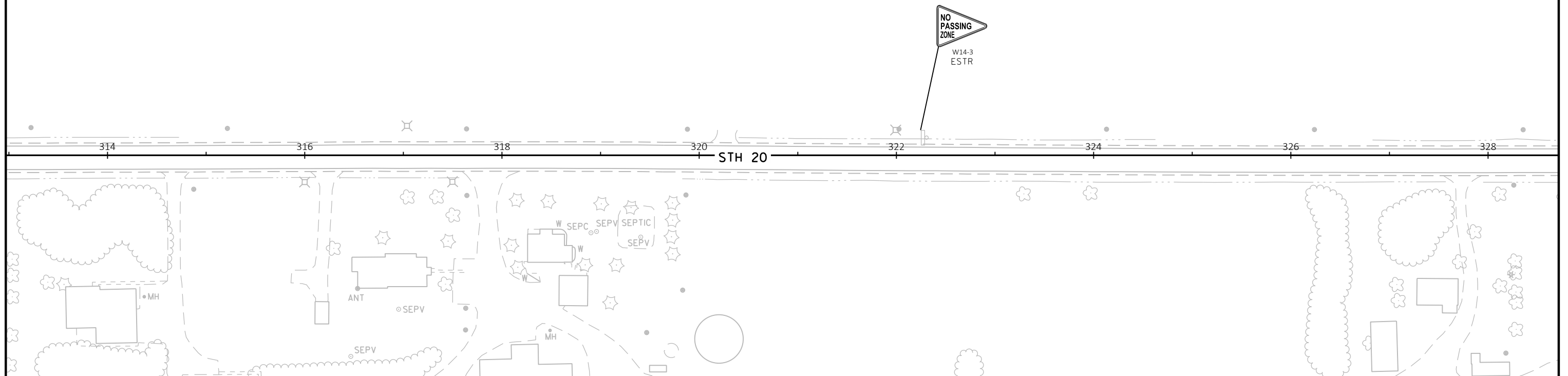
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NOTE:
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PAY ITEM. PLACE SIGNS BASED ON WHERE THE PAVEMENT MARKING FOR
NO PASSING ZONES BEGIN.



SHEET 21 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH











PERMANENT SIGNING

SHEET

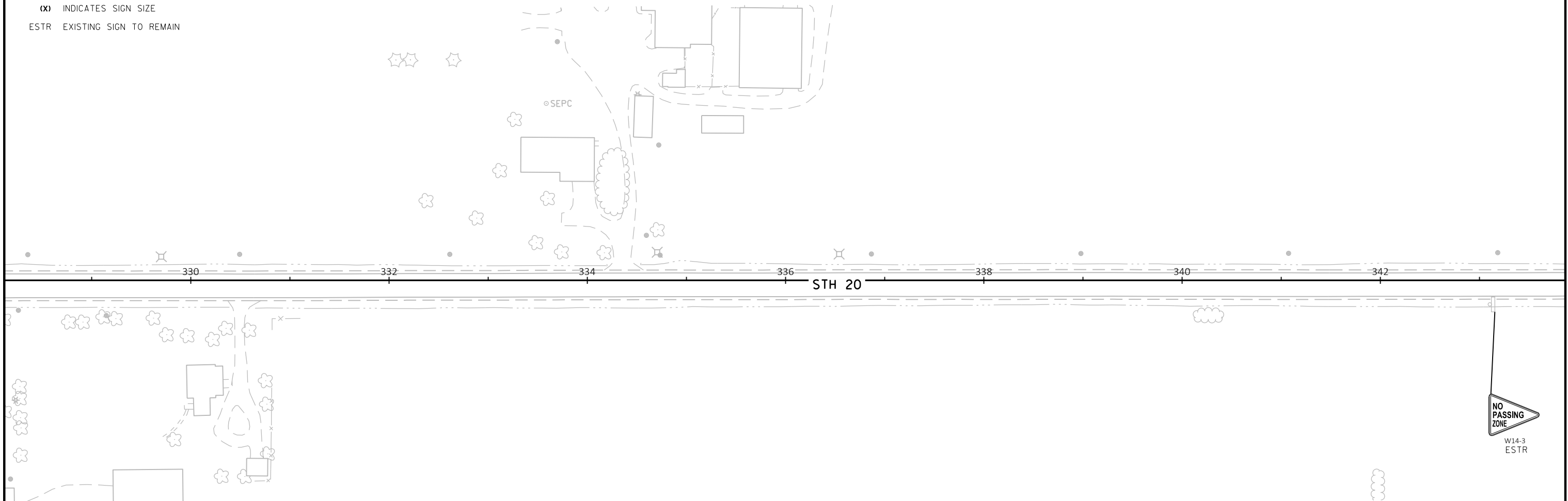
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LEGEND

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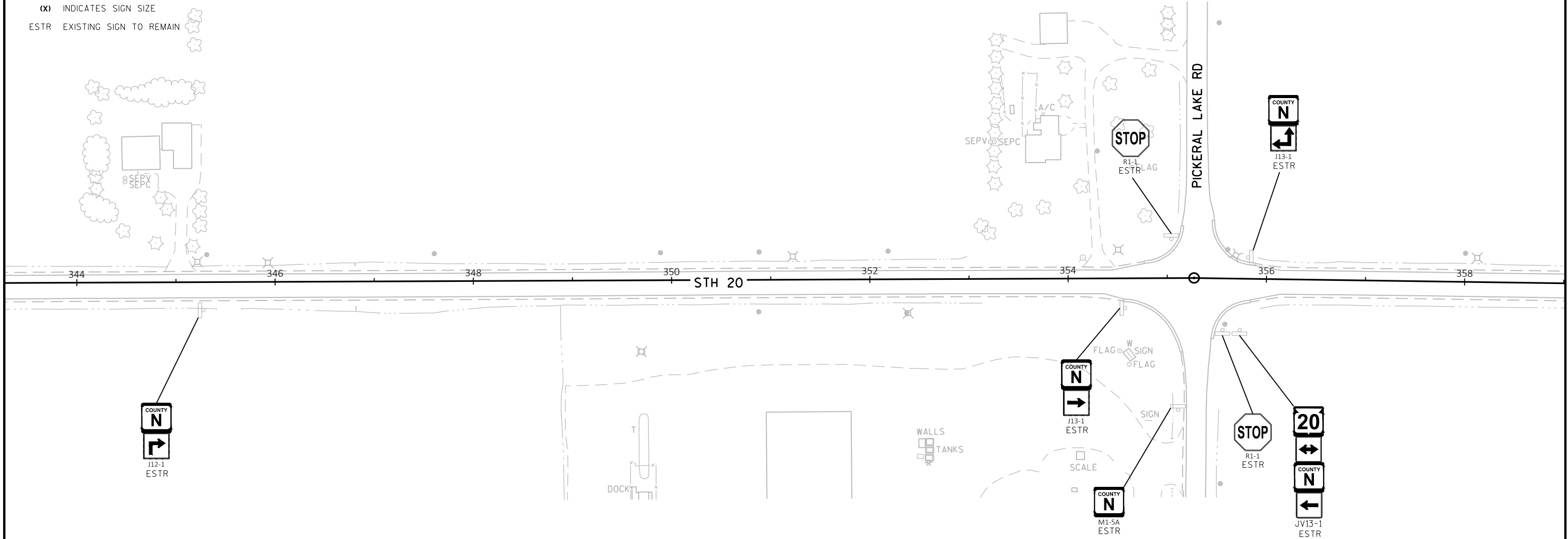
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

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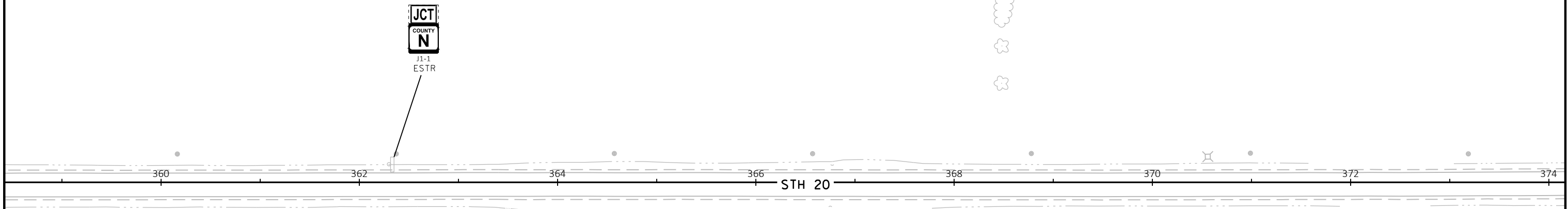
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




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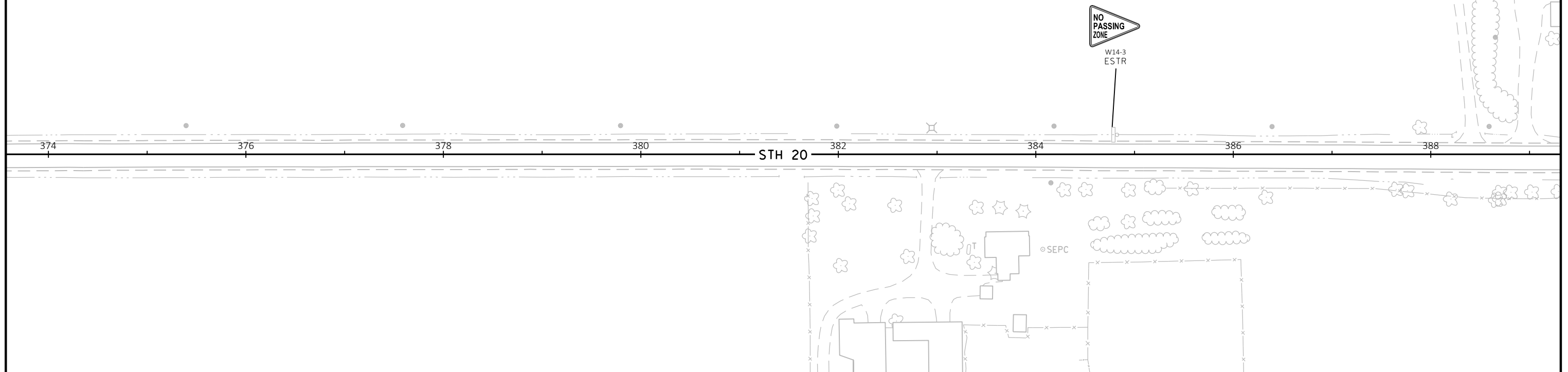
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SHEET 25 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

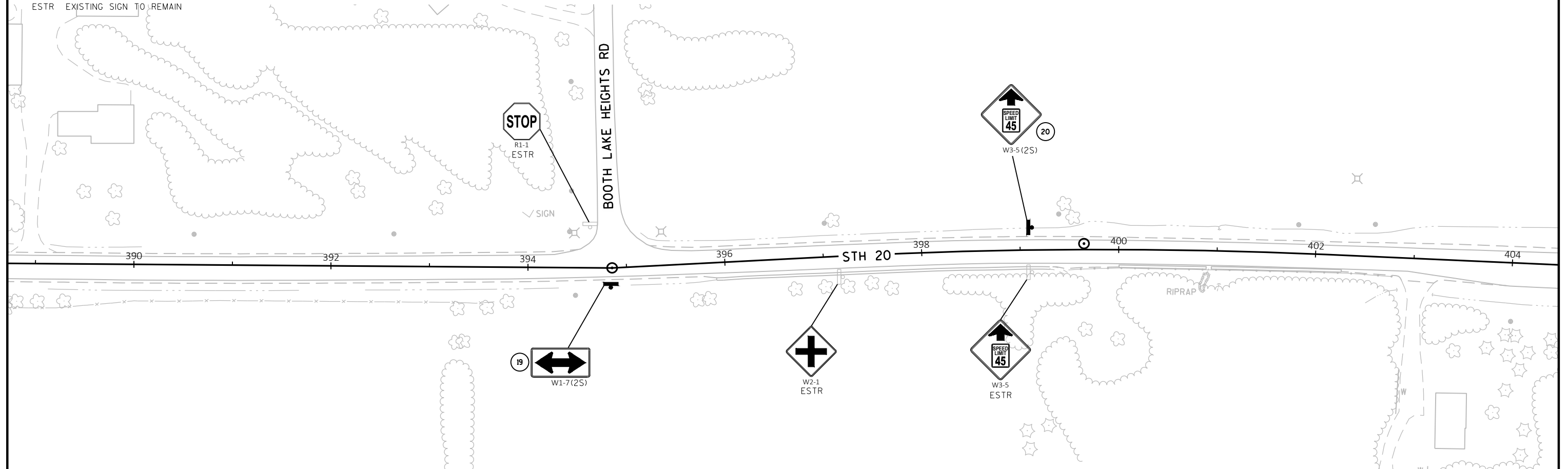
PERMANENT SIGNING

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**LEGEND**

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SHEET 26 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

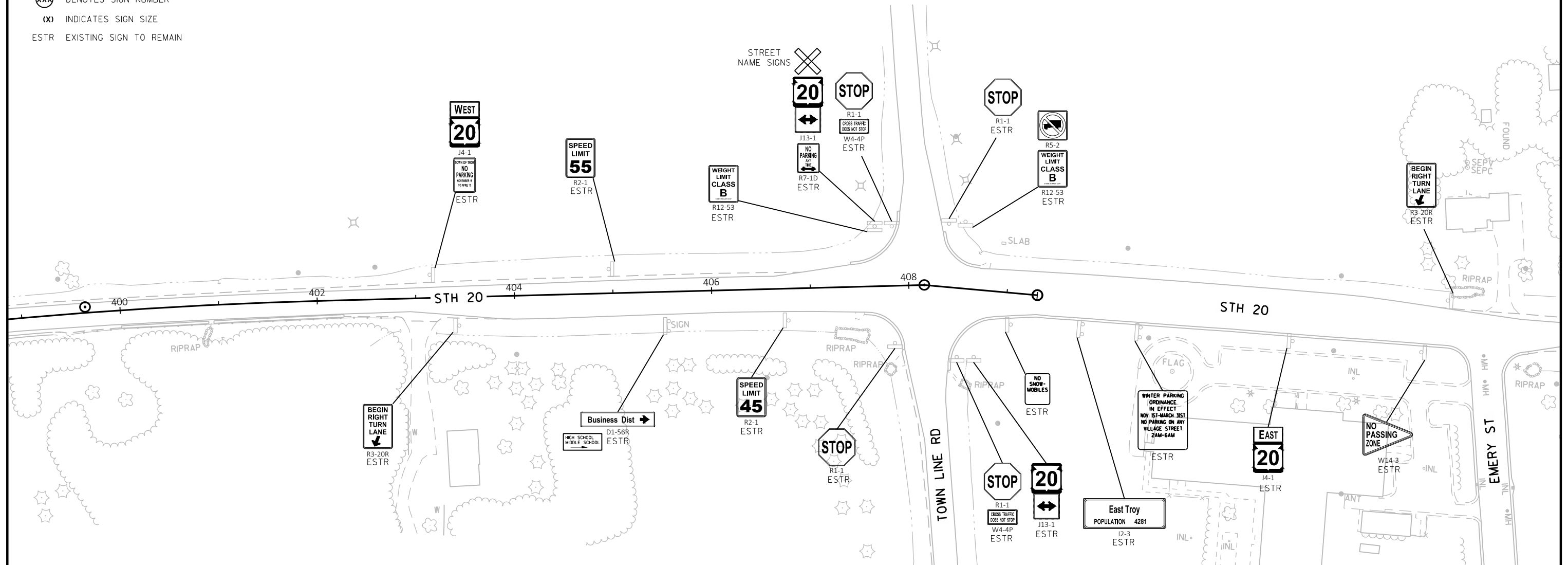
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LEGEND

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SHEET 27 OF 27

PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

PERMANENT SIGNING

SHEET

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FILE NAME : N:\PDS\C3D\CAD\31300630\SIGN\023201_PS.DWG
LAYOUT NAME - SHEET-27

PLOT DATE : 1/25/2019 1:38 PM


PLOT BY : MARTENS, KAREN L

PLOT NAME :

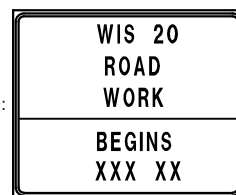
PLOT SCALE : 1 IN:100 FT

WISDOT/CADDs SHEET 42

LEGEND

- PCMS** TRAFFIC CONTROL PCMS (PORTABLE CHANGEABLE MESSAGE SIGN)
-  WORK AREA
- 1 SEE S.D.D. 15C04-05 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC
- 2 SEE S.D.D. 15C05-05 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC
- 3 SEE S.D.D. 15C04-05 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC (TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL)
- 4 SEE S.D.D. 15C12-07 TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING. USE AS NEEDED FOR CONSTRUCTION OPERATIONS.

PORTABLE CHANGEABLE MESSAGE SIGNS SHOULD READ:



GENERAL NOTES

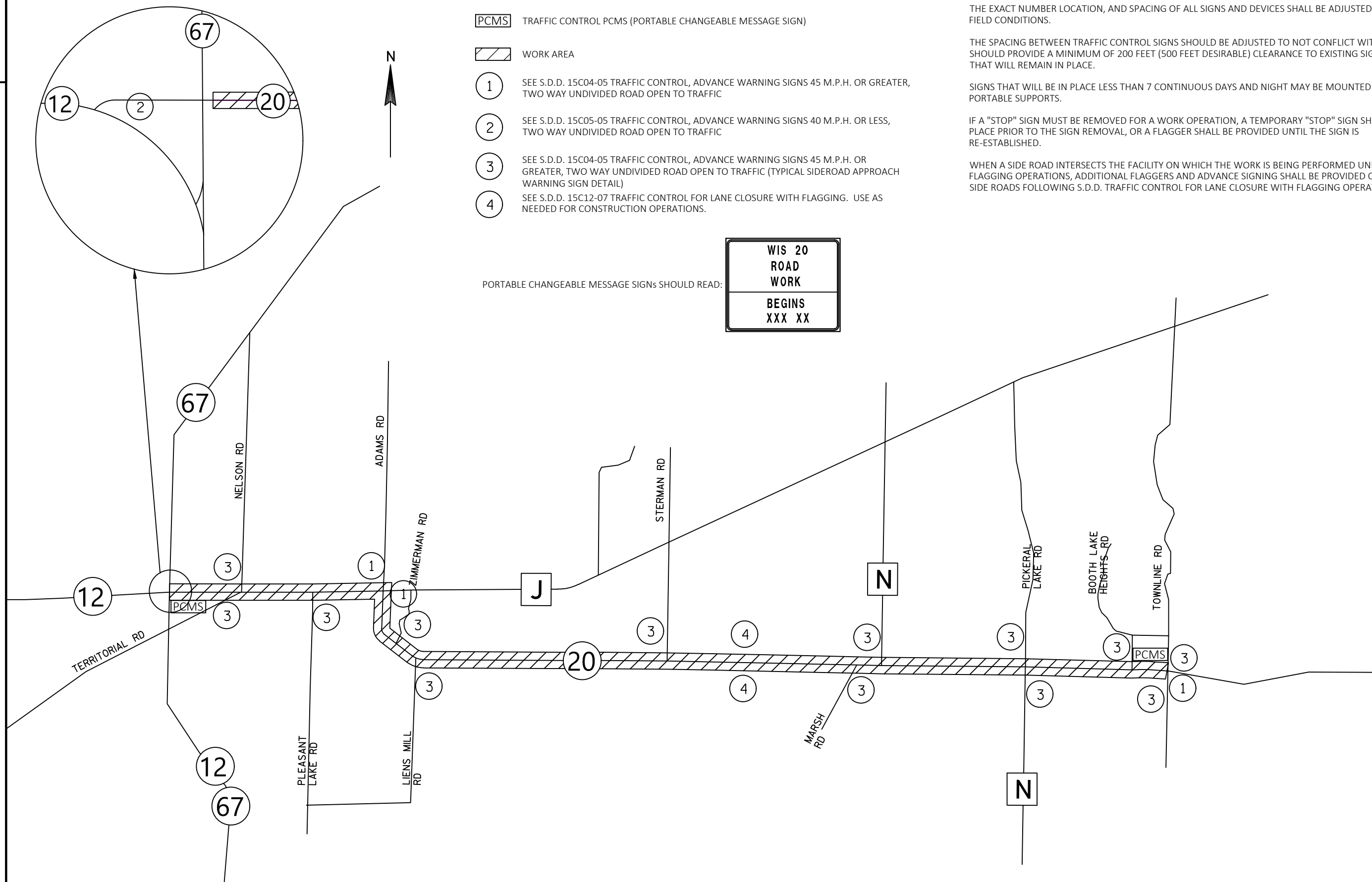
THE EXACT NUMBER LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

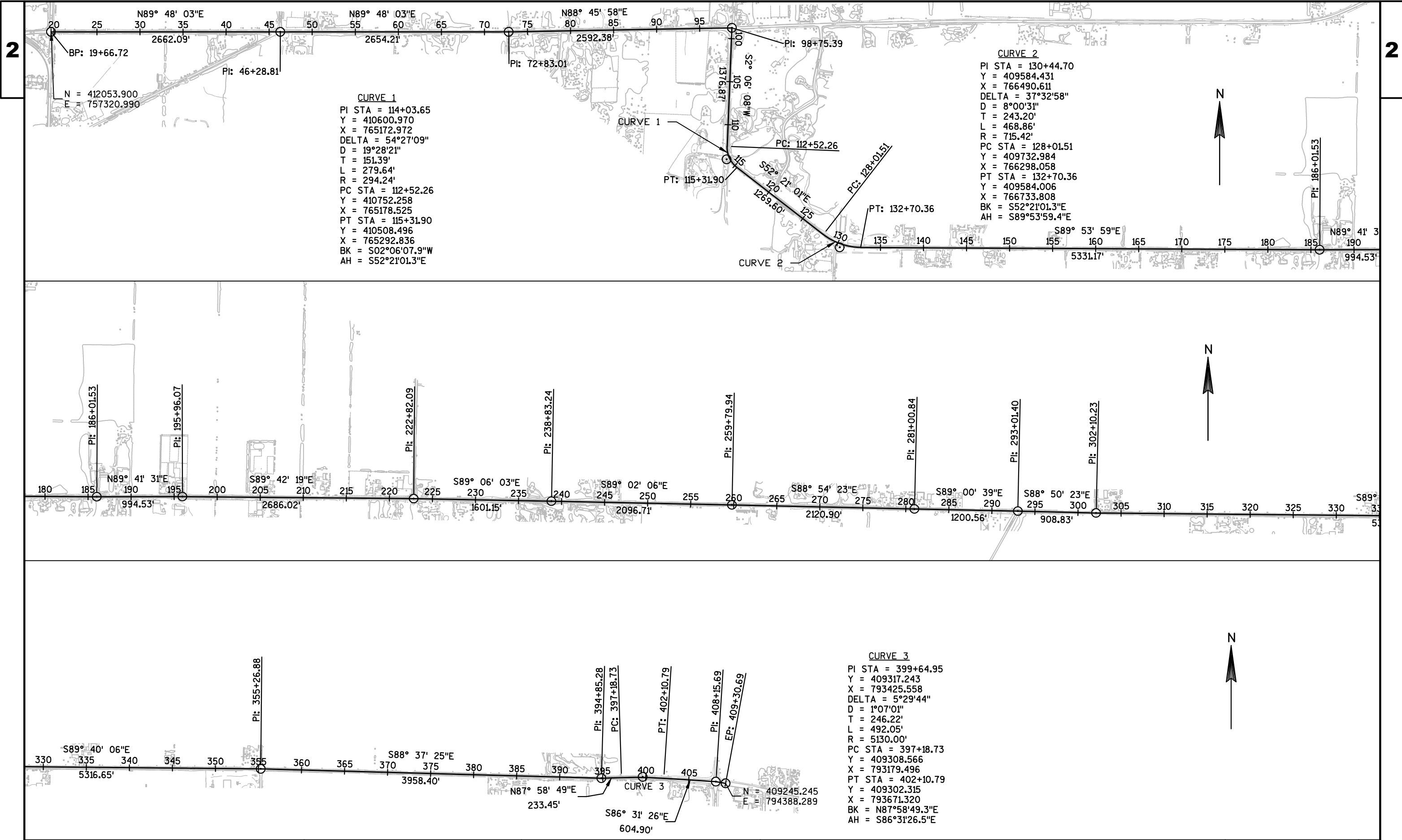
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHT MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACE PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

WHEN A SIDE ROAD INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED UNDER FLAGGING OPERATIONS, ADDITIONAL FLAGGERS AND ADVANCE SIGNING SHALL BE PROVIDED ON SIDE ROADS FOLLOWING S.D.D. TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION.





PROJECT NO: 3130-06-60

HWY: STH 20

COUNTY: WALWORTH

ALIGNMENT DETAIL

SHEET

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Estimate Of Quantities

3130-06-60

| Line | Item | Item Description | Unit | Total | Qty |
|------|------------|---------------------------------------------------------------|------|-------------|-------------|
| 0002 | 204.0120 | Removing Asphaltic Surface Milling | SY | 130,946.000 | 130,946.000 |
| 0004 | 213.0100 | Finishing Roadway (project) 01. 3130-06-60 | EACH | 1.000 | 1.000 |
| 0006 | 305.0110 | Base Aggregate Dense 3/4-Inch | TON | 3,462.000 | 3,462.000 |
| 0008 | 305.0500 | Shaping Shoulders | STA | 776.000 | 776.000 |
| 0010 | 390.0203 | Base Patching Asphaltic | SY | 777.000 | 777.000 |
| 0012 | 455.0605 | Tack Coat | GAL | 9,113.000 | 9,113.000 |
| 0014 | 460.0105.S | HMA Percent Within Limits (PWL) Test Strip Volumetrics | EACH | 1.000 | 1.000 |
| 0016 | 460.0110.S | HMA Percent Within Limits (PWL) Test Strip Density | EACH | 1.000 | 1.000 |
| 0018 | 460.2005 | Incentive Density PWL HMA Pavement | DOL | 13,765.000 | 13,765.000 |
| 0020 | 460.2007 | Incentive Density HMA Pavement Longitudinal Joints | DOL | 15,886.000 | 15,886.000 |
| 0022 | 460.2010 | Incentive Air Voids HMA Pavement | DOL | 17,283.000 | 17,283.000 |
| 0024 | 460.5224 | HMA Pavement 4 LT 58-28 S | TON | 17,283.000 | 17,283.000 |
| 0026 | 465.0120 | Asphaltic Surface Driveways and Field Entrances | TON | 168.000 | 168.000 |
| 0028 | 465.0475 | Asphalt Centerline Rumble Strips 2-Lane Rural | LF | 34,300.000 | 34,300.000 |
| 0030 | 618.0100 | Maintenance And Repair of Haul Roads (project) 01. 3130-06-60 | EACH | 1.000 | 1.000 |
| 0032 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0034 | 624.0100 | Water | MGAL | 52.000 | 52.000 |
| 0036 | 634.0618 | Posts Wood 4x6-Inch X 18-FT | EACH | 15.000 | 15.000 |
| 0038 | 637.2210 | Signs Type II Reflective H | SF | 14.920 | 14.920 |
| 0040 | 637.2230 | Signs Type II Reflective F | SF | 117.000 | 117.000 |
| 0042 | 638.2102 | Moving Signs Type II | EACH | 7.000 | 7.000 |
| 0044 | 638.3000 | Removing Small Sign Supports | EACH | 6.000 | 6.000 |
| 0046 | 642.5001 | Field Office Type B | EACH | 1.000 | 1.000 |
| 0048 | 643.0300 | Traffic Control Drums | DAY | 2,880.000 | 2,880.000 |
| 0050 | 643.0900 | Traffic Control Signs | DAY | 4,334.000 | 4,334.000 |
| 0052 | 643.1050 | Traffic Control Signs PCMS | DAY | 14.000 | 14.000 |
| 0054 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |
| 0056 | 646.1020 | Marking Line Epoxy 4-Inch | LF | 5,400.000 | 5,400.000 |
| 0058 | 646.1040 | Marking Line Grooved Wet Ref Epoxy 4-Inch | LF | 73,405.000 | 73,405.000 |
| 0060 | 646.3020 | Marking Line Epoxy 8-Inch | LF | 593.000 | 593.000 |
| 0062 | 646.4520 | Marking Line Same Day Epoxy 4-Inch | LF | 46,539.000 | 46,539.000 |
| 0064 | 646.5020 | Marking Arrow Epoxy | EACH | 2.000 | 2.000 |
| 0066 | 646.5120 | Marking Word Epoxy | EACH | 1.000 | 1.000 |
| 0068 | 646.6120 | Marking Stop Line Epoxy 18-Inch | LF | 142.000 | 142.000 |
| 0070 | 648.0100 | Locating No-Passing Zones | MI | 7.360 | 7.360 |
| 0072 | 649.0120 | Temporary Marking Line Epoxy 4-Inch | LF | 5,400.000 | 5,400.000 |
| 0074 | 649.0150 | Temporary Marking Line Removable Tape 4-Inch | LF | 5,400.000 | 5,400.000 |
| 0076 | 649.0770 | Temporary Marking Raised Pavement Marker Type II | EACH | 820.000 | 820.000 |
| 0078 | 650.8000 | Construction Staking Resurfacing Reference | LF | 38,965.000 | 38,965.000 |

Estimate Of Quantities

3130-06-60

| Line | Item | Item Description | Unit | Total | Qty |
|------|----------|--------------------------------------------------------------------|------|------------|------------|
| 0080 | 650.9910 | Construction Staking Supplemental Control (project) 01. 3130-06-60 | LS | 1.000 | 1.000 |
| 0082 | 740.0440 | Incentive IRI Ride | DOL | 29,458.000 | 29,458.000 |
| 0084 | ASP.1T0A | On-the-Job Training Apprentice at \$5.00/HR | HRS | 300.000 | 300.000 |
| 0086 | ASP.1T0G | On-the-Job Training Graduate at \$5.00/HR | HRS | 250.000 | 250.000 |
| 0088 | SPV.0060 | Special 01. Section Corner Monuments | EACH | 14.000 | 14.000 |
| 0090 | SPV.0180 | Special 01. Resin Binder High Friction Surface Treatment Modified | SY | 5,483.000 | 5,483.000 |

| ASPHALTIC SURFACE | | | | | | | | | |
|-------------------|------------------------------------------|----------------------------------|----------------------|-------------------------------|-----------|--------------------------------|-------------------------------------|------------------------------------------|----------|
| | REMOVING ASPHALTIC SURFACE MILLING | BASE AGGREGATE DENSE 3/4-INCH | SHAPING SHOULDERS | BASE PATCHING ASPHALTIC | TACK COAT | HMA PAVMENT TYPE 4LT 58-28S | DRIVEWAYS AND FIELD ENTRANCES | ASPHALTIC CENTERLINE RUMBLE STRIPS | WATER |
| | 204.0120 | 305.0110 | 305.0500 | 390.0203 | 455.0605 | 460.5224 | 465.0120 | 465.0475 | 624.0100 |
| LOCATION | SY | TON | STA | SY | GAL | TON | TON | LF | MGAL |
| 20+45 TO 98+75 | 26257 | 703 | 158 | 38 | 1827 | 3465 | 35 | 6575 | 11 |
| 98+75 TO 409+30 | 104689 | 2759 | 618 | 740 | 7286 | 13818 | 133 | 27725 | 41 |
| TOTAL | 130946 | 3462 | 776 | 777 | 9113 | 17283 | 168 | 34300 | 52 |

| ASPHALT PWL | | |
|---------------------|-----------------------------------------------|------------------------------------------|
| | HMA PAVEMENT PWL TEST STRIP VOLUMETRICS | HMA PAVEMENT PWL TES STRIP DENSITY |
| | 460.0105.S | 460.0110.S |
| LOCATION | EACH | EACH |
| STA 20+45 TO 409+30 | 1 | 1 |
| TOTAL | 1 | 1 |

| TRAFFIC CONTROL | | | | | | | |
|-----------------|----------|------|----------|------|------------|-----|-----------------------------------------------------|
| | 643.0300 | | 643.0900 | | 643.1050 | | 649.0770 |
| | DRUMS | | SIGNS | | SIGNS PCMS | | TEMPORARY MARKING RAISED PAVEMENT MARKER TYPE II |
| | EACH | DAY | EACH | DAY | EACH | DAY | EACH |
| 3130-06-60 | 484 | 2880 | 128 | 4334 | 2 | 14 | 820 |
| TOTAL | | 2880 | | 4334 | | 14 | 820 |

NOTE: UNLESS OTHERWISE NOTED, ALL ITEMS ARE CATEGORY 0010

PWL Mixture Use Table

The following acceptance criteria are applicable for this project:

| Location | Station | Mixture Use | Underlying Surface | Bid Item | Tons | Thickness | Quality Management Program to be used for: | |
|------------------------|-----------------|-------------------------------|-----------------------------|-------------------|-------|-----------|-----------------------------------------------|---------------------------------------------|
| | | | | | | | Mixture Acceptance | Density Acceptance |
| Mainline | 20+45 to 98+75 | Upper Layer | Milled Existing HMA Surface | 4LT 58-28S | 2772 | 2 " | PWL Incentive Air Voids HMA Pavement 460.2010 | Incentive Density PWL HMA Pavement 460.2005 |
| Shoulders & Turn Lanes | 20+45 to 98+75 | Upper Layer | Milled Existing HMA Surface | 4LT 58-28S | 693 | 2 " | PWL Incentive Air Voids HMA Pavement 460.2010 | QMP as per SS 460 |
| Mainline | 98+75 to 409+30 | Upper Layer | Milled Existing HMA Surface | 4LT 58-28S | 10993 | 2 " | PWL Incentive Air Voids HMA Pavement 460.2010 | Incentive Density PWL HMA Pavement 460.2005 |
| Shoulders & Turn Lanes | 98+75 to 409+30 | Upper Layer | Milled Existing HMA Surface | 4LT 58-28S | 2825 | 2 " | PWL Incentive Air Voids HMA Pavement 460.2010 | QMP as per SS 460 |
| Various | | Base Patch | Milled Existing HMA Surface | Asphaltic Surface | 103 | 6" | QMP as per SS 390 | Acceptance by ordinary compaction |
| Various | | Driveways and Field Entrances | Milled Existing HMA Surface | Asphaltic Surface | 164 | 4" | QMP as per SS 465 | Acceptence by ordinary compaction |

NOTE: UNLESS OTHERWISE NOTED, ALL ITEMS ARE CATEGORY 0010

| PAVEMENT MARKINGS | | | | | | | | | |
|-------------------|----------------------|--------------|----------|-----------------------|-------------|----------|----------|-----------|---------------|
| | 646.1040 | 646.3020 | | 646.4520 | | 646.5020 | 646.5120 | 646.6120 | 648.0100 |
| | MARKING LINE GROOVED | MARKING LINE | | MARKING LINE SAME DAY | | MARKING | MARKING | MARKING | LOCATE NO |
| | WET REF EPOXY 4-INCH | EPOXY 8-INCH | | EPOXY 4-INCH | | ARROW | WORD | STOP LINE | PASSING ZONES |
| | (WHITE) | (WHITE) | (WHITE) | (YELLOW) | (YELLOW) | (WHITE) | (WHITE) | (WHITE) | |
| | | | 3' SKIPS | | 12.5' SKIPS | | "ONLY" | | |
| LOCATION | LF | LF | LF | LF | LF | EACH | EACH | LF | |
| 20+40 TO 98+75 | 15285 | 307 | 53 | 11683 | 866 | 2 | 1 | 24 | 1.48 |
| 98+75 TO 409+30 | 58121 | 158 | 75 | 29651 | 4,339 | -- | -- | 118 | 5.88 |
| TOTAL | 73,405 | 593 | | 46,539 | | 2 | 1 | 142 | 7.36 |

| PAVEMENT MARKINGS HFST | | | | | | |
|------------------------|--------------|----------|------------------------|----------|------------------------|----------|
| | 646.1020 | | 649.0120 | | 649.0150 | |
| | MARKING LINE | | TEMPORARY MARKING LINE | | TEMPORARY MARKING LINE | |
| | EPOXY 4-INCH | | EPOXY 4-INCH | | REMOVABLE TAPE 4-INCH | |
| | (WHITE) | (YELLOW) | (WHITE) | (YELLOW) | (WHITE) | (YELLOW) |
| LOCATION | LF | LF | LF | LF | LF | LF |
| HFST AREA | | | | | | |
| STA 109+00 TO 117+00 | 800 | 1600 | 800 | 1600 | 800 | 1600 |
| STA 125+00 TO 135+00 | 1000 | 2000 | 1000 | 2000 | 1000 | 2000 |
| TOTAL | 5,400 | | 5,400 | | 5,400 | |

*NOTE: ADDITIONAL PAVEMENT MARKING MOBILZATIONS REQUIRED

NOTE: UNLESS OTHERWISE NOTED, ALL ITEMS ARE CATEGORY 0010

HIGH FRICTION SURFACE TREATMENT

RESIN BINDER
HFST MODIFIED

SPV.0180.01

| LOCATION | SY |
|------------------|------|
| 110+10 TO 117+60 | 2250 |
| 125+00 TO 134+70 | 3233 |
| TOTAL | 5483 |

CONSTRUCTION STAKING

650.8000

650.9910

RESURFACING
REFERENCE

SUPPLEMENTAL CONTROL
(3130-06-60)

| PROJECT I.D. | DESCRIPTION | LF | LS |
|--------------|-------------|-------|----|
| 3130-06-60 | USH 20 | 38965 | 1 |
| TOTAL | | 38965 | 1 |

SECTION CORNER MONUMENTS

SPV.0060.01

| LOCATION | EACH |
|------------|------|
| STA 46+29 | 1 |
| STA 72+83 | 1 |
| STA 98+72 | 1 |
| STA 143+25 | 1 |
| STA 169+89 | 1 |
| STA 196+48 | 1 |
| STA 222+82 | 1 |
| STA 249+32 | 1 |
| STA 275+74 | 1 |
| STA 302+10 | 1 |
| STA 328+74 | 1 |
| STA 355+27 | 1 |
| STA 381+67 | 1 |
| STA 408+16 | 1 |
| TOTAL | 14 |

FIELD OFFICE

642.5001

TYPE B

| PROJECT I.D. | DESCRIPTION | EACH |
|--------------|-------------|------|
| 3130-06-60 | USH 20 | 1 |
| TOTAL | | 1 |

NOTE: UNLESS OTHERWISE NOTED, ALL ITEMS ARE CATEGORY 0010

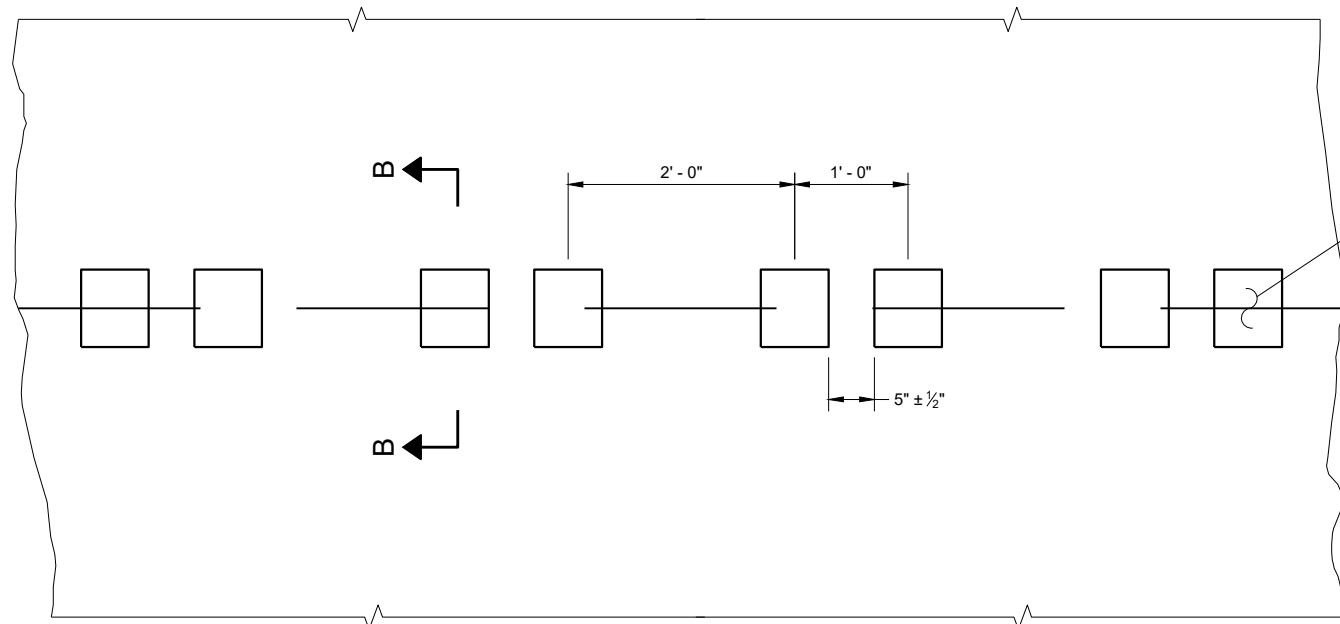
TYPE II PERMANENT SIGNING -

3130-06-60 STH 20

| SIGN NO. | SIGN CODE & SIZE | SIGN MESSAGE | SIGN SIZE W x H [IN.] x [IN.] | 637.2210 SIGNS TYPE II REFLC H [SF] | 637.2230 SIGNS TYPE II REFLC F [SF] | 638.3000 REMOVING SMALL SIGN SUPPORTS [EA] | 634.0618 WOOD POSTS 4"X6"x18' [EA] | 638.2102 MOVE SIGNS TYPE II [EA] | MOUNT ON SAME POST AS SIGN # | REMARKS / NEW SIGN LOCATION |
|---------------------------------|------------------|--------------|-------------------------------------|----------------------------------------------------|----------------------------------------------------|-----------------------------------------------------------|------------------------------------------------|----------------------------------------------|---------------------------------------|-----------------------------|
| 1 | R1-1(3) | -- | 36 x 36 | 7.460 | -- | -- | 1 | -- | -- | SHEET 1A |
| 2 | R1-1(3) | -- | 36 x 36 | 7.460 | -- | -- | 1 | -- | -- | SHEET 1A |
| 3 | W1-7(2S) | -- | 48 x 24 | -- | 8.000 | -- | 1 | -- | -- | SHEET 3 |
| 4 | W1-7(2S) | -- | 48 x 24 | -- | 8.000 | -- | 1 | -- | -- | SHEET 5 |
| 5 | R1-2 | -- | -- x -- | -- | -- | -- | 1 | 1 | -- | SHEET 6 |
| 6 | W1-10L(2M) | -- | 36 x 36 | -- | 9.000 | -- | -- | -- | -- | SHEET 8 |
| 7 | W13-1(3) | -- | 24 x 24 | -- | 4.000 | -- | -- | -- | #6 | -- |
| 8 | W1-10L(2M) | -- | 36 x 36 | -- | 9.000 | -- | -- | -- | -- | -- |
| 9 | W13-1(3) | -- | 24 x 24 | -- | 4.000 | -- | -- | -- | #8 | -- |
| 10 | W1-7(2S) | -- | 48 x 24 | -- | 8.000 | -- | -- | -- | -- | -- |
| 11 | W1-7(2S) | -- | 48 x 24 | -- | 8.000 | -- | -- | -- | -- | -- |
| 12 | W1-10R(2M) | -- | 36 x 36 | -- | 9.000 | -- | -- | -- | -- | SHEET 9 |
| 13 | W13-1(3) | -- | 24 x 24 | -- | 4.000 | -- | -- | -- | #12 | -- |
| 14 | W1-10R(2M) | -- | 36 x 36 | -- | 9.000 | -- | -- | -- | -- | -- |
| 15 | W13-1(3) | -- | 24 x 24 | -- | 4.000 | -- | -- | -- | #14 | -- |
| 16 | W1-7(2S) | -- | 48 x 24 | -- | 8.000 | -- | 1 | -- | -- | SHEET 15 |
| 17 | W1-7(2S) | -- | 48 x 24 | -- | 8.000 | -- | 1 | -- | -- | SHEET 19 |
| 18 | J13-1 | -- | -- x -- | -- | -- | 1 | 1 | 1 | -- | SHEET 20 |
| 19 | W1-7(2S) | -- | 48 x 24 | -- | 8.000 | -- | 1 | -- | -- | SHEET 26 |
| 20 | W3-5(2S) | 45 MPH | 36 x 36 | -- | 9.000 | -- | 1 | -- | -- | -- |
| -- | -- | -- | -- x -- | -- | -- | -- | -- | -- | -- | -- |
| UNDISTRIBUTED (NO PASSING ZONE) | | | -- x -- | -- | -- | 5 | 5 | 5 | -- | -- |
| TOTALS | | | | 14.920 | 117.000 | 6 | 15 | 7 | | |

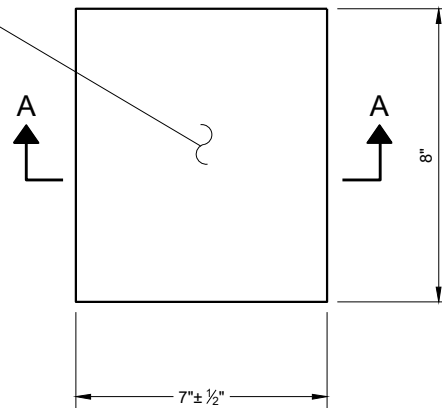
Standard Detail Drawing List

| | |
|-----------|------------------------------------------------------------------------------------------------------|
| 13A11-03A | 2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING |
| 13A11-03B | 2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING |
| 14B29-01 | SAFETY EDGE |
| 15C04-05 | TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC |
| 15C05-05 | TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS |
| 15C07-15B | PAVEMENT MARKING WORDS |
| 15C07-15C | PAVEMENT MARKING ARROWS |
| 15C08-20A | LONGITUDINAL MARKING (MAINLINE) |
| 15C08-20C | PAVEMENT MARKING (TURN LANES) |
| 15C11-07B | CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS |
| 15C12-07 | TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION |
| 15C19-06A | MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY |
| 15C33-04 | STOP LINE AND CROSSWALK PAVEMENT MARKING |
| 15C34-03 | STANDARD APPLICATION FOR TEMPORARY RAISED PAVEMENT MARKER, TYPE 2 |
| 15D28-03 | TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY |
| 15D38-02A | TEMPORARY TRAFFIC CONTROL SIGN MOUNTING |
| 15D38-02B | ATTACHMENT OF SIGNS TO POSTS |



PLAN VIEW
SHOULDER WITH GROOVES

PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP



PLAN VIEW
(SINGLE GROOVE)

GENERAL NOTES

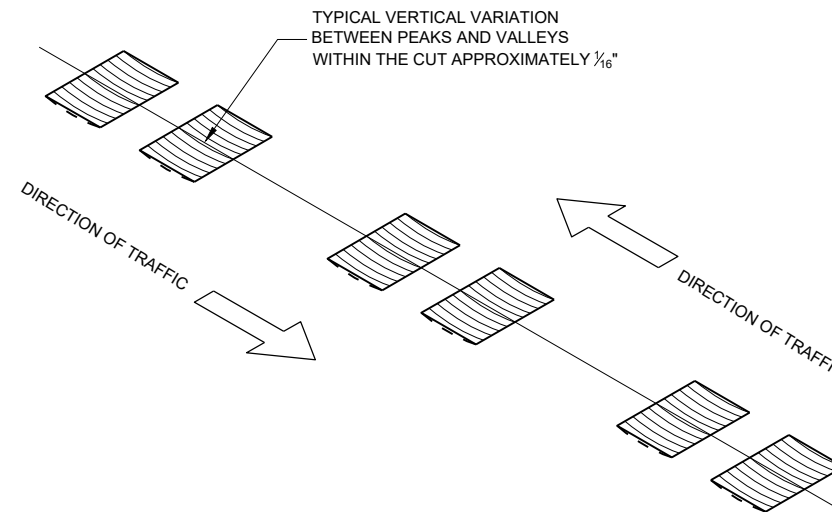
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

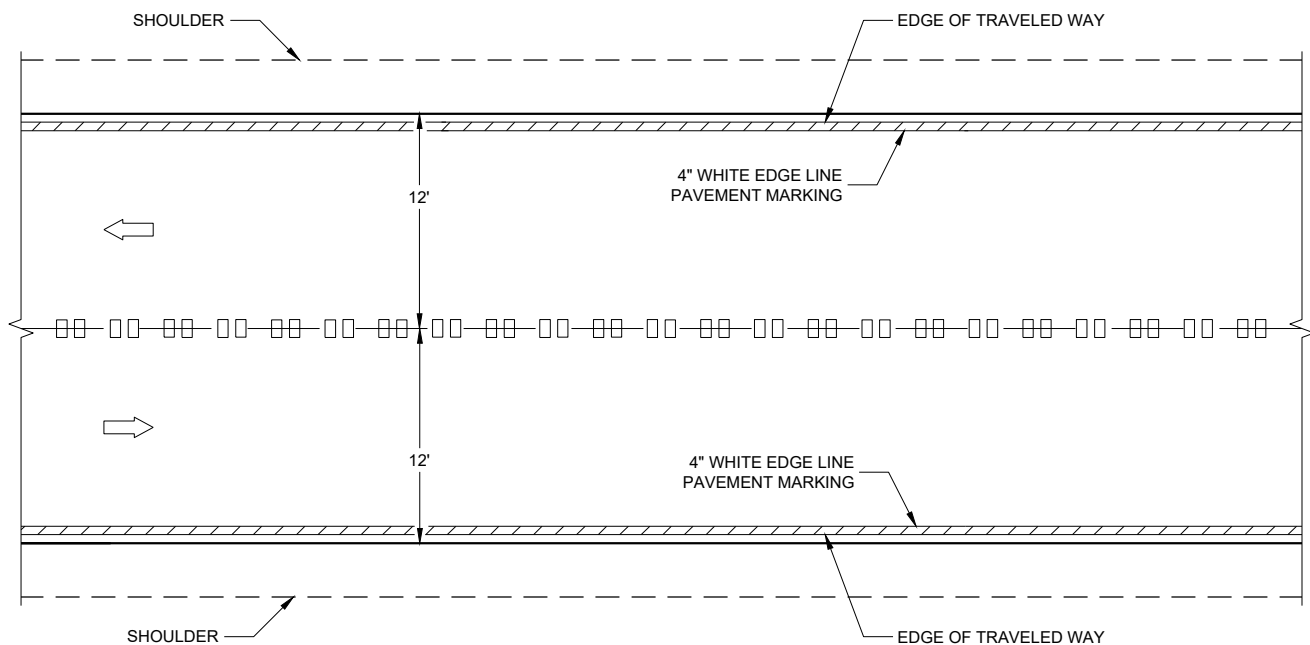
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

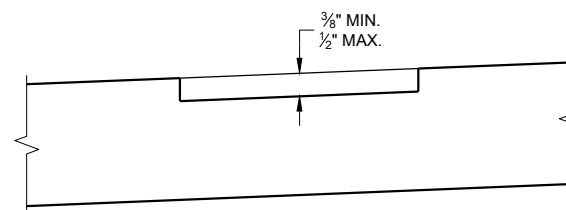
- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



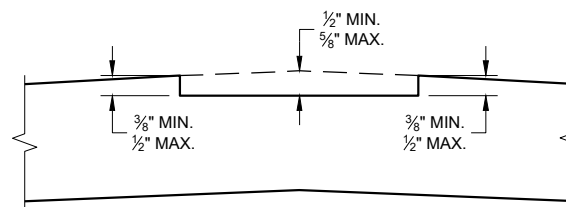
ISOMETRIC



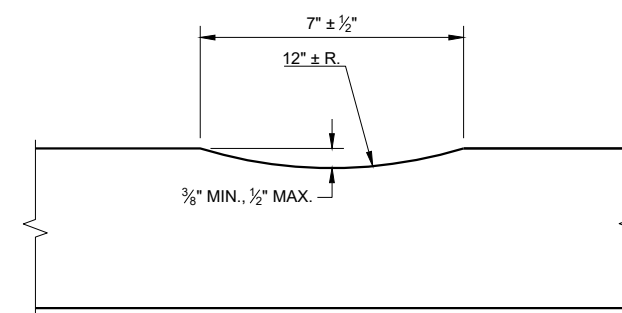
CENTERLINE GROOVES ON TWO-WAY ROADWAYS



SECTION B - B
SUPERELEVATED ROADWAY



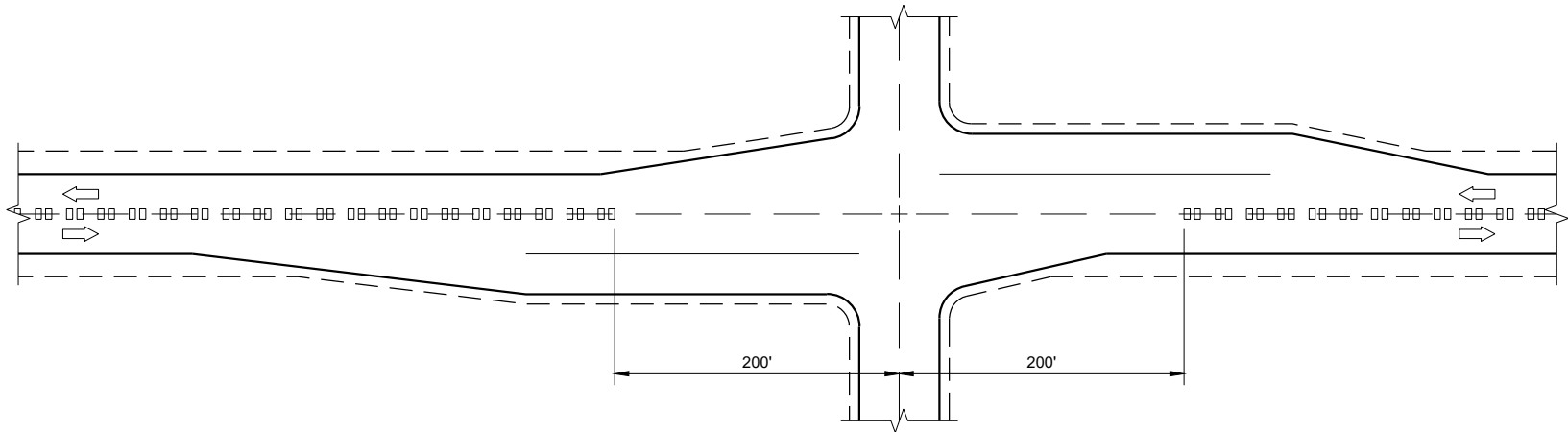
SECTION B - B
CROWNED ROADWAY



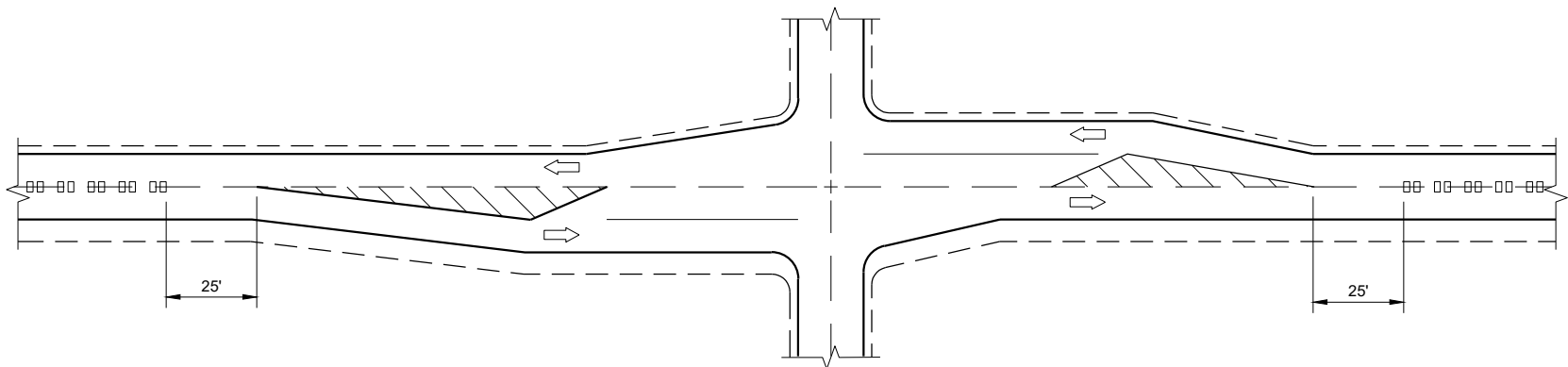
SECTION A - A

2-LANE RURAL
CENTER LINE RUMBLE STRIP,
MILLING

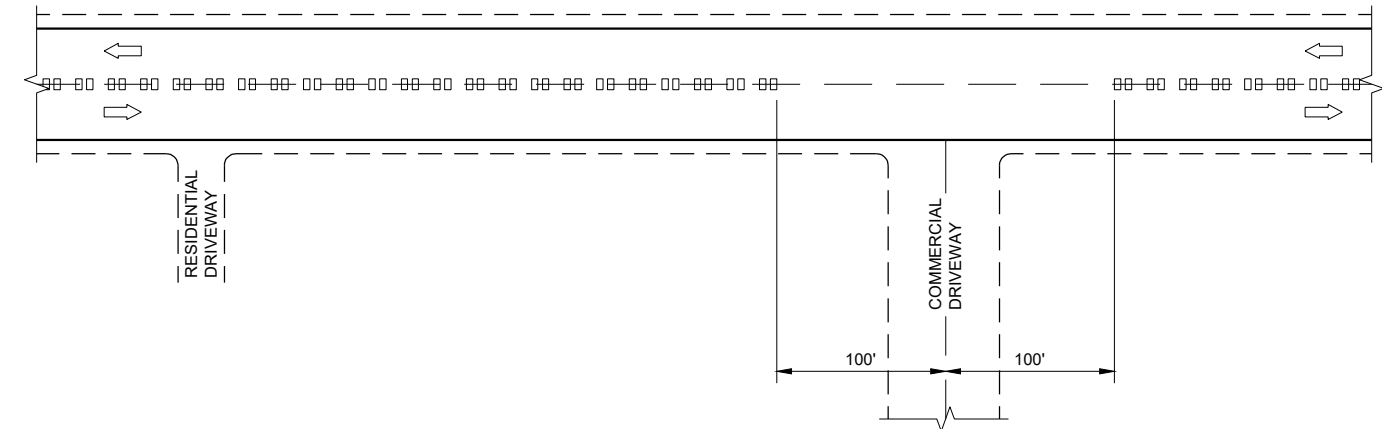
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CENTERLINE GROOVES AT INTERSECTIONS



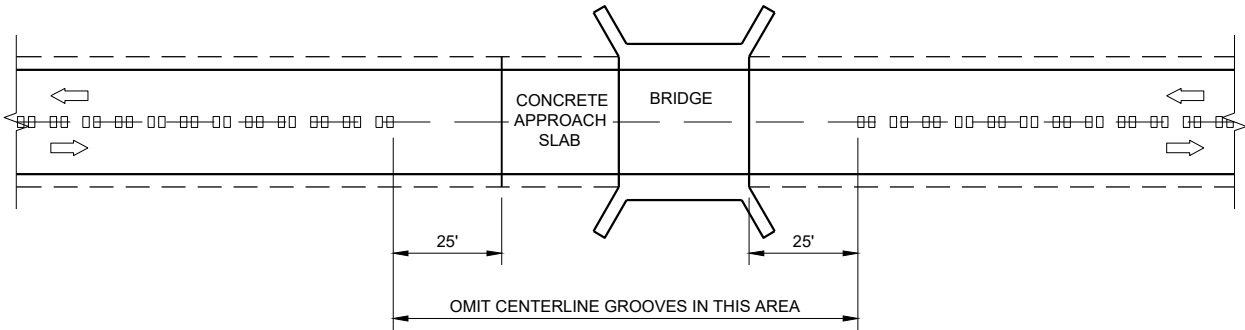
CENTERLINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)



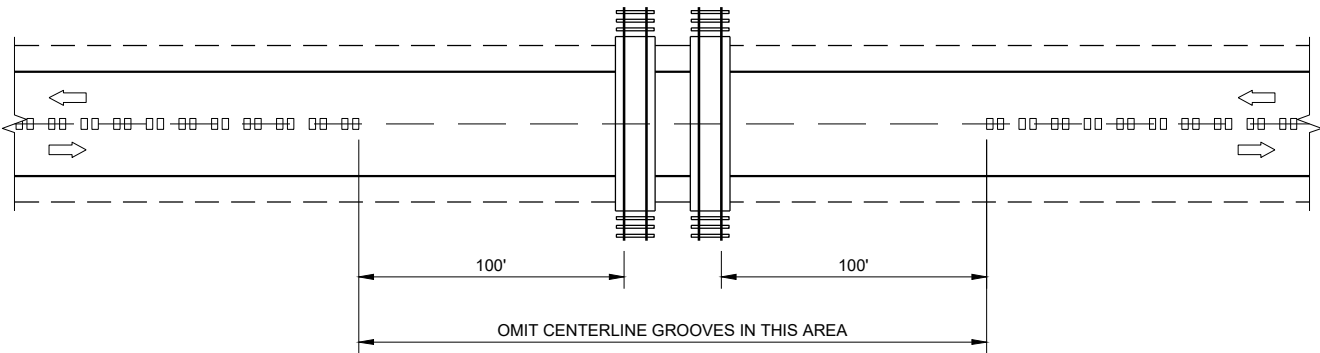
CENTERLINE GROOVES AT DRIVEWAYS^①

GENERAL NOTES

- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

2-LANE RURAL
CENTERLINE RUMBLE STRIP,
MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

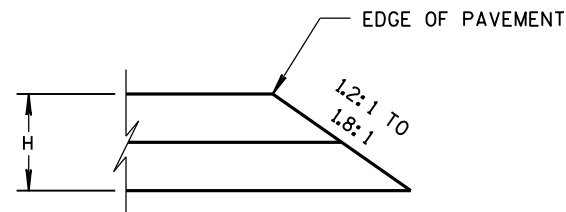
APPROVED

7/2018

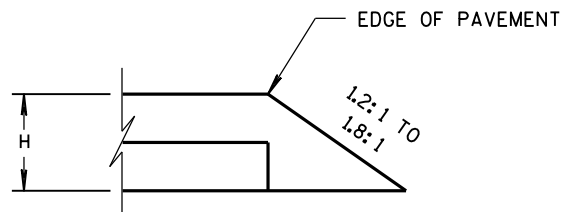
DATE

FHWA

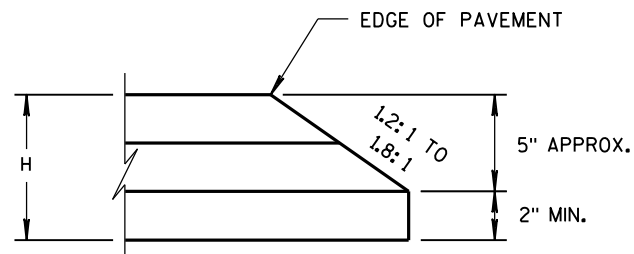
/S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



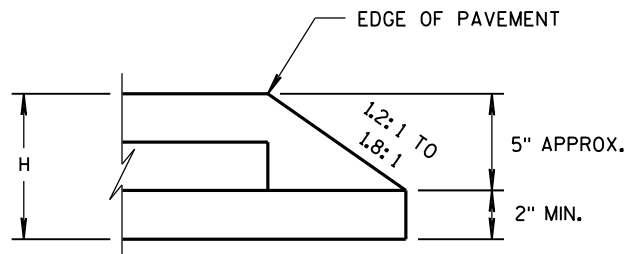
CONSTRUCTED WITH FINAL TWO LAYERS
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER
FOR H 5" OR LESS

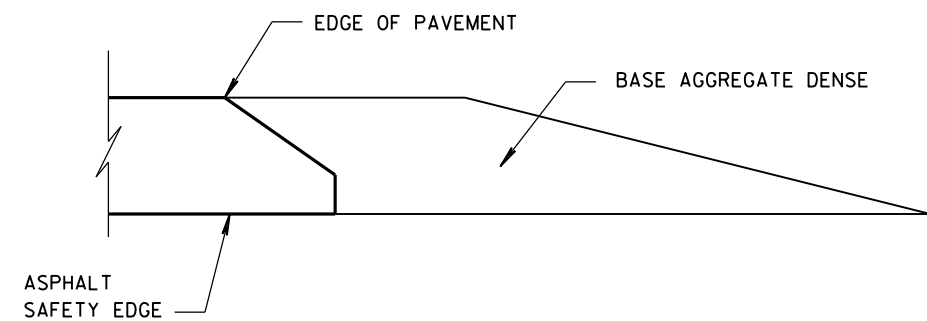


CONSTRUCTED WITH FINAL TWO LAYERS
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

SAFETY EDGE_{SM}

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

11/30/2012
DATE

FHWA

/s/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

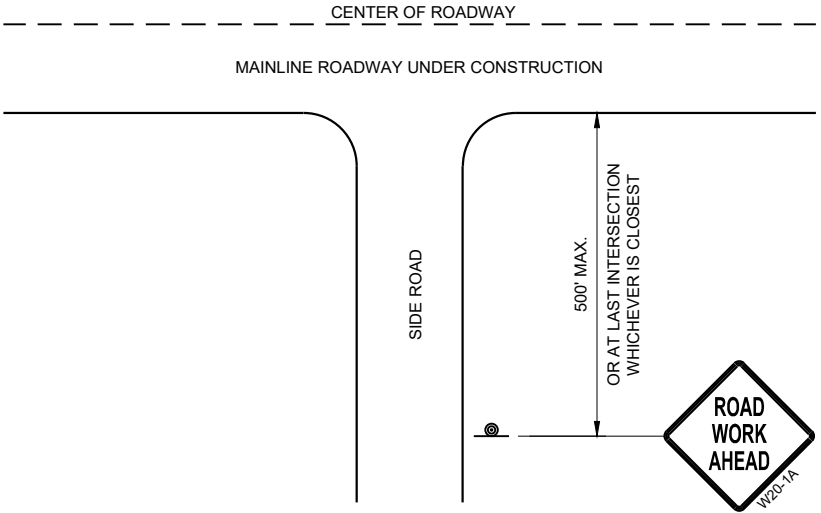
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

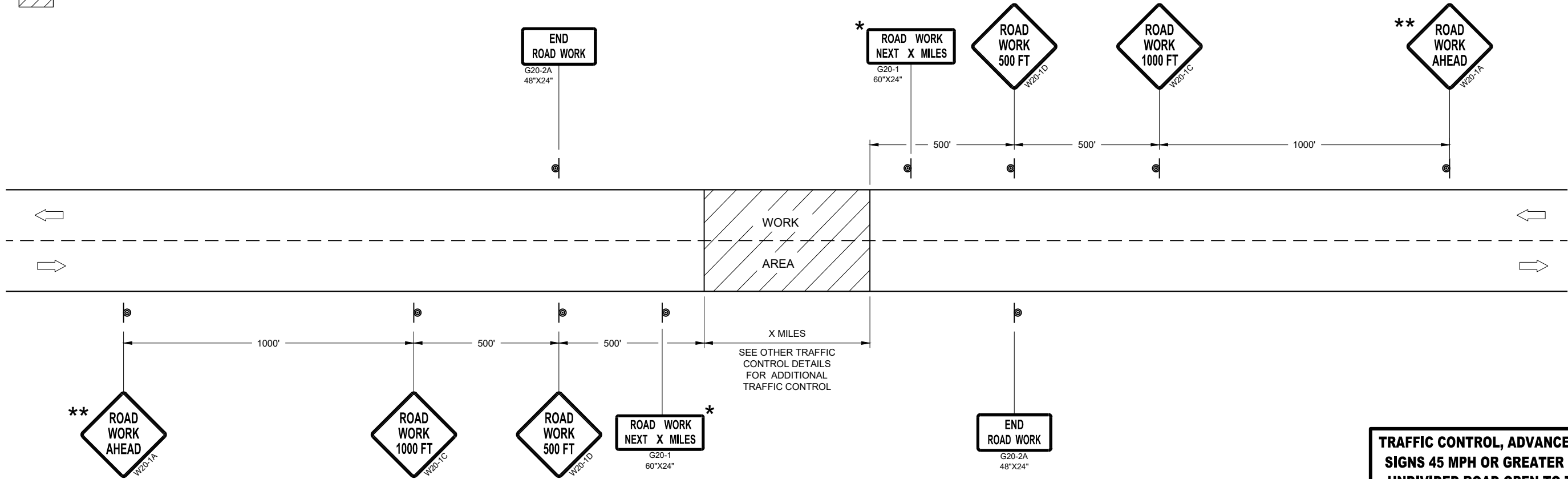
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

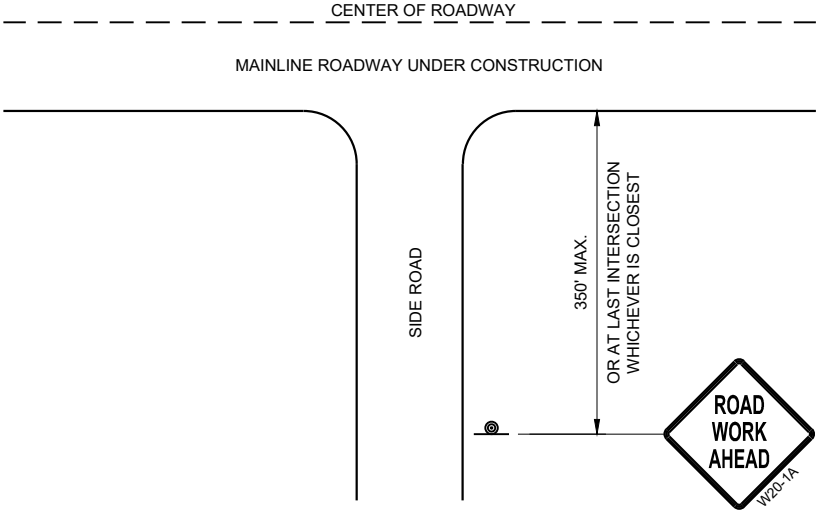
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

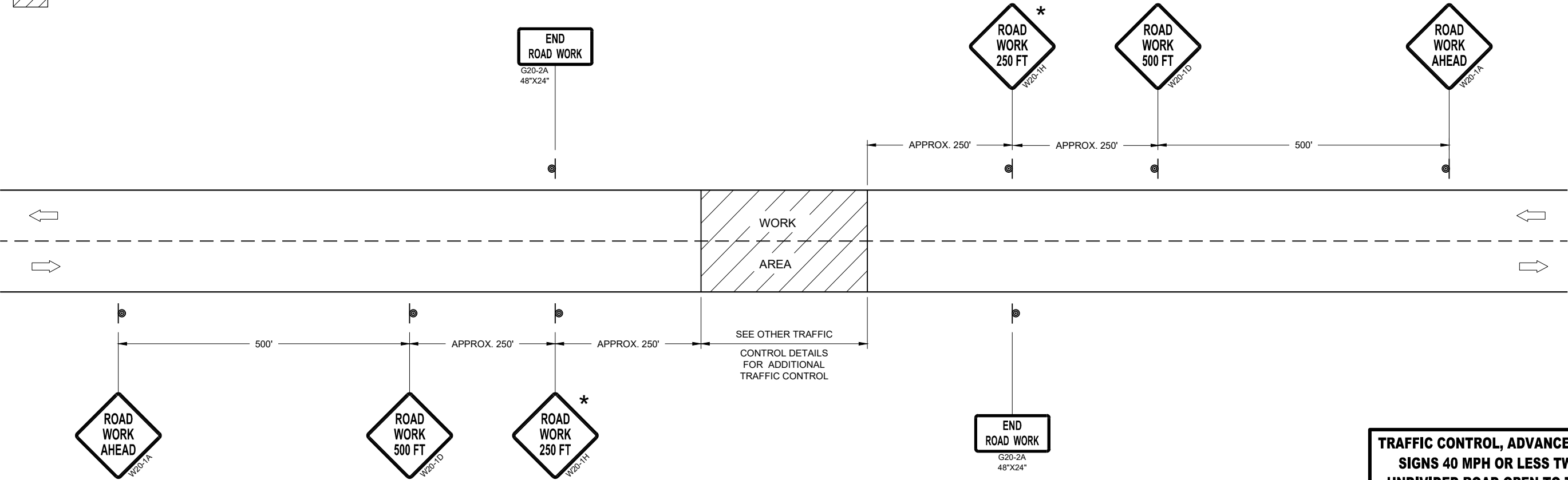
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL

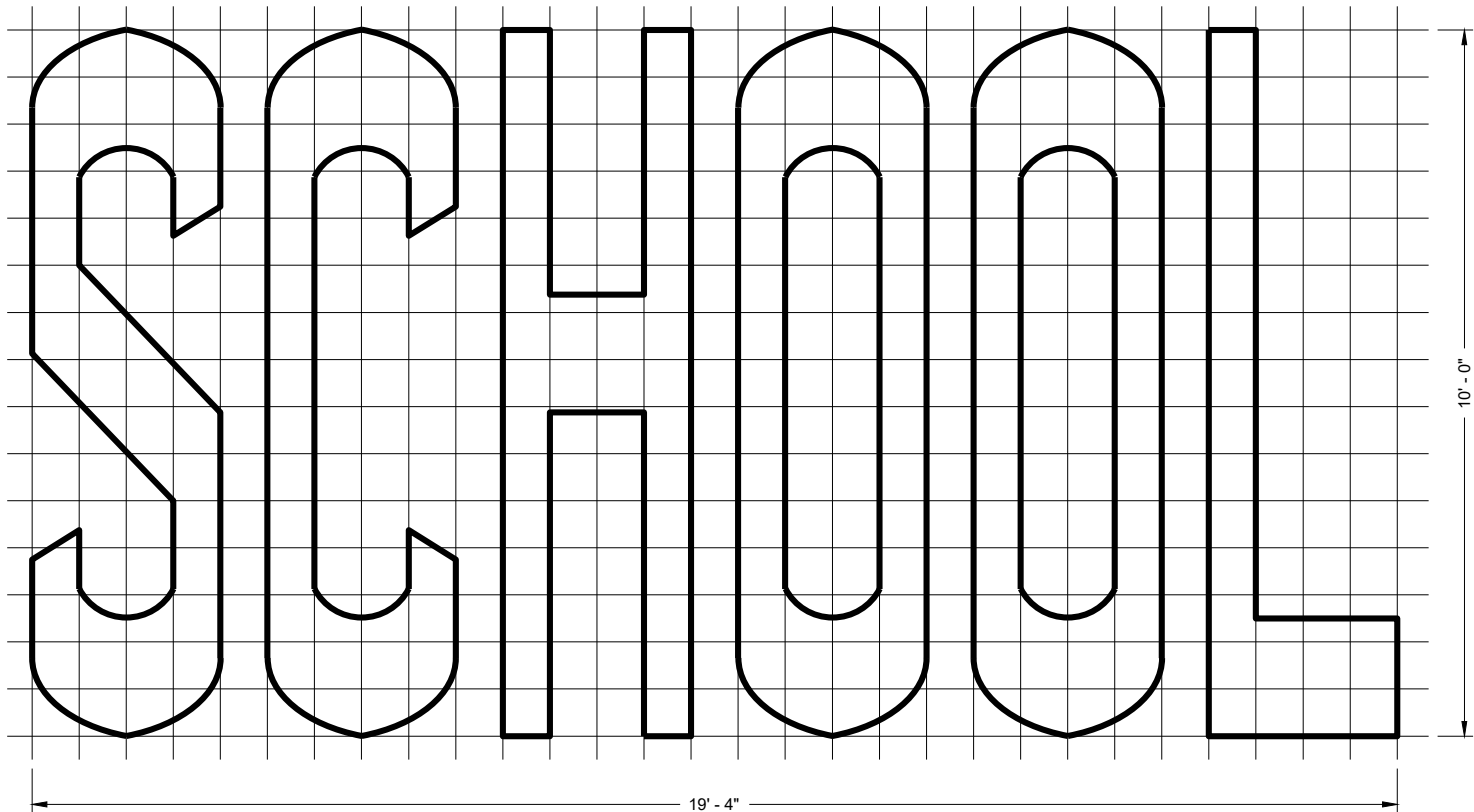
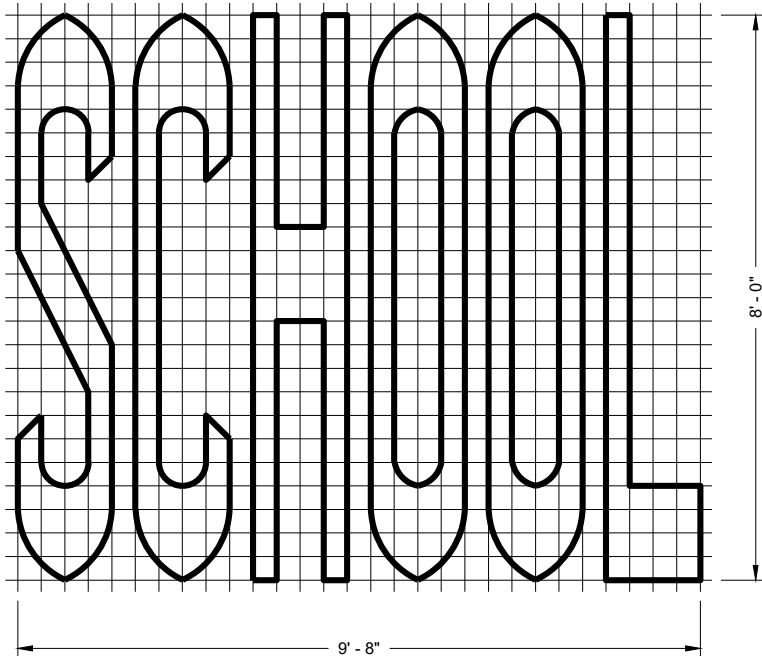
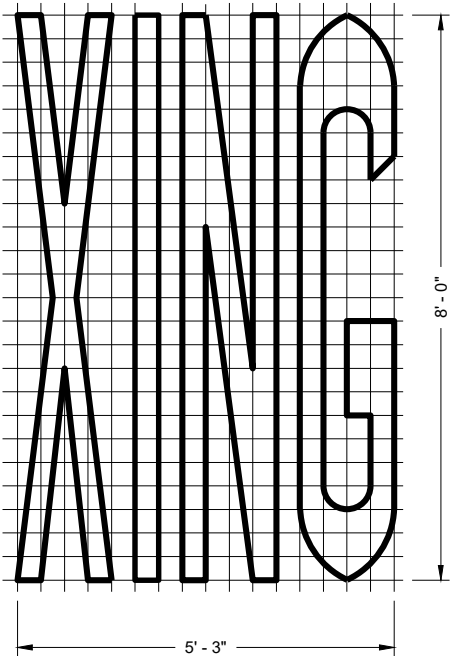
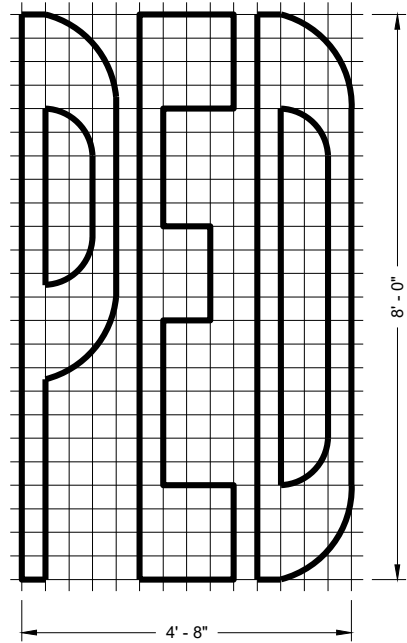
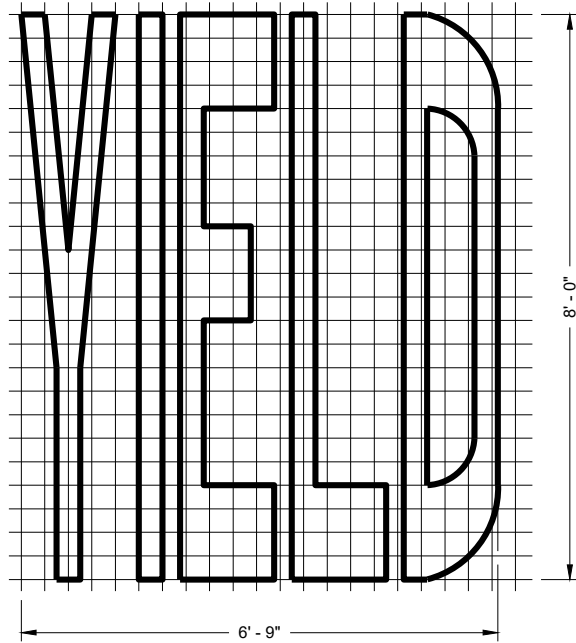
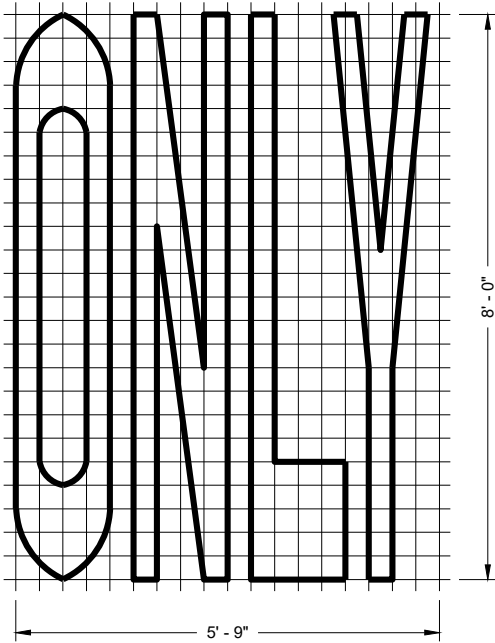
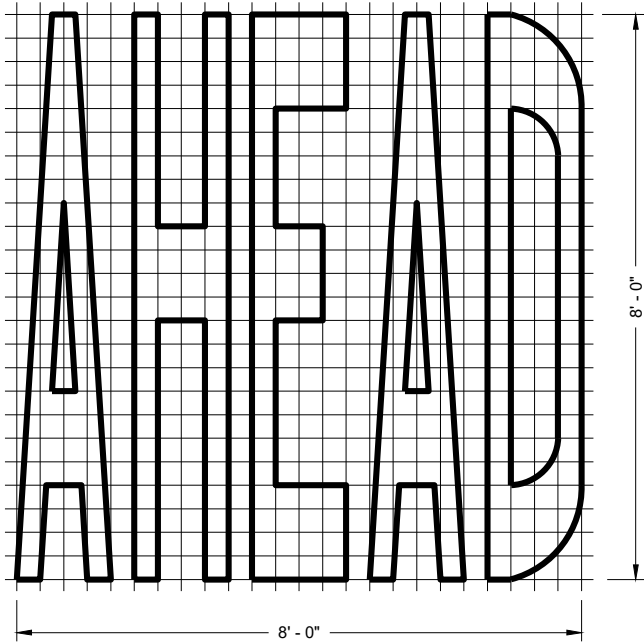
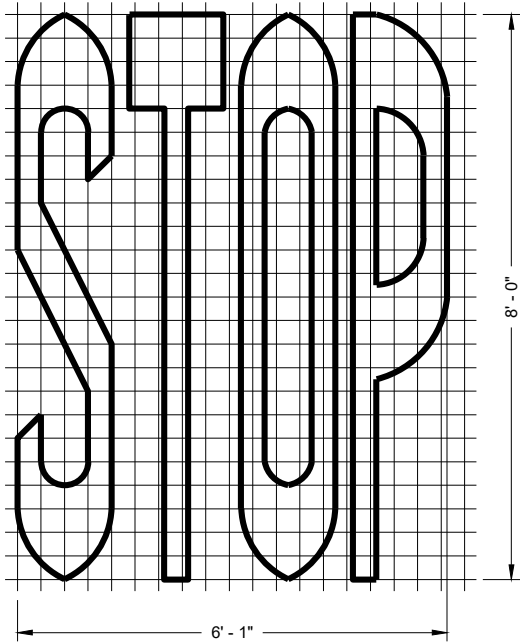


TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



SINGLE LANE

TWO - LANE

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

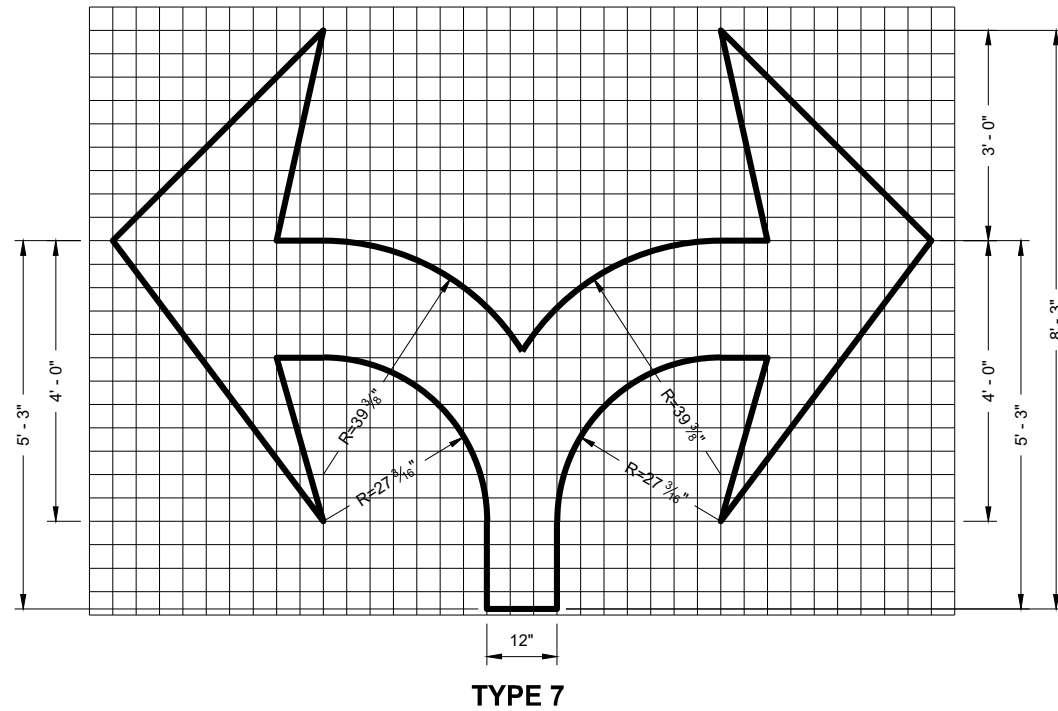
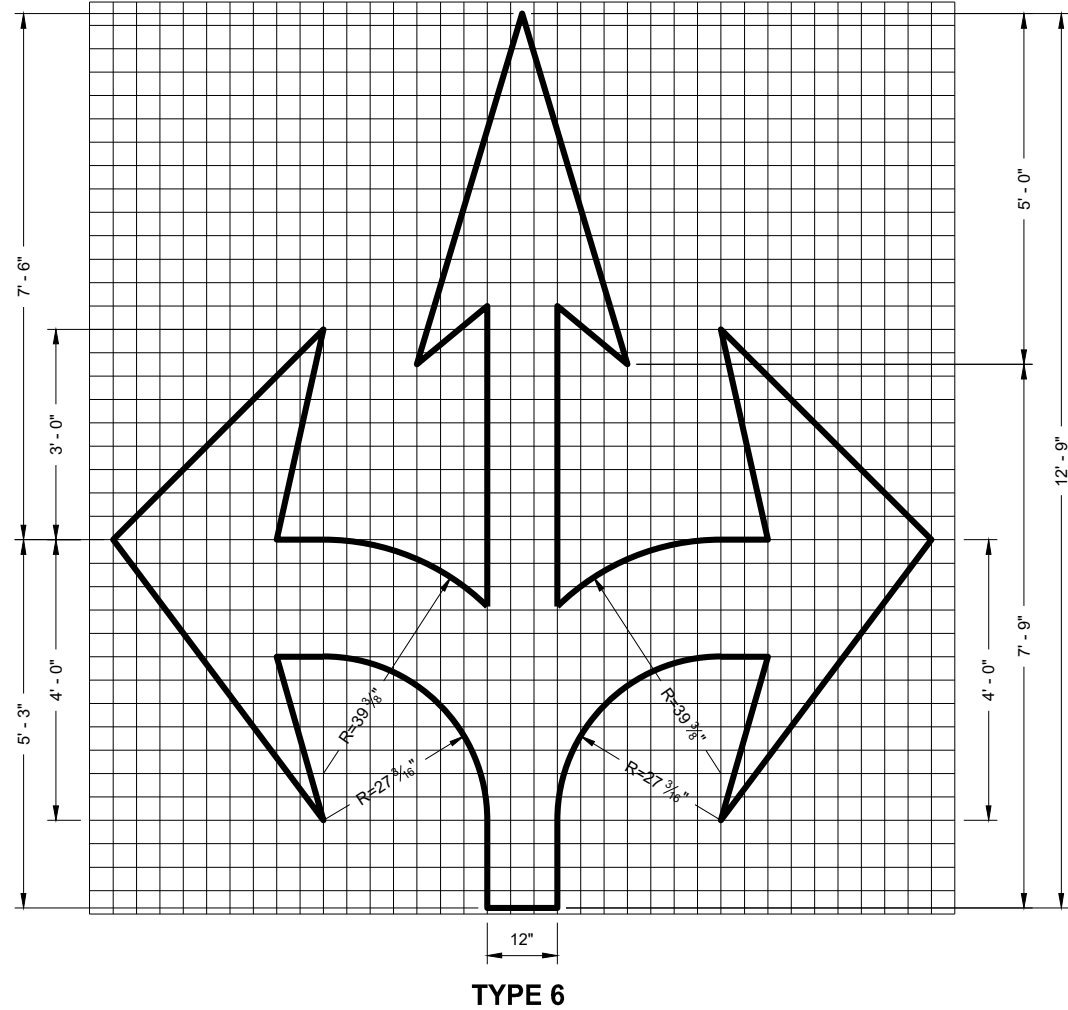
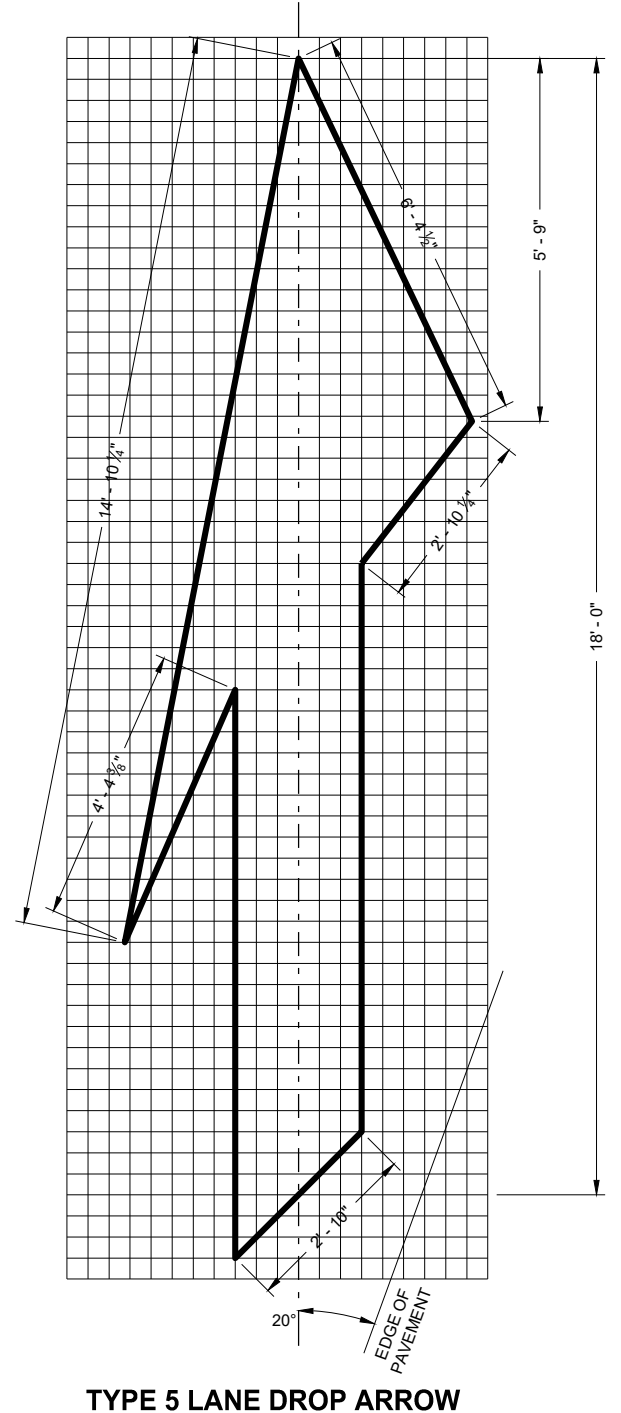
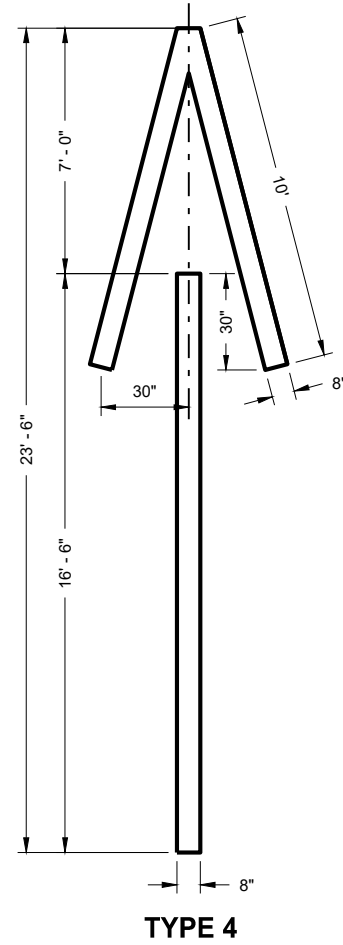
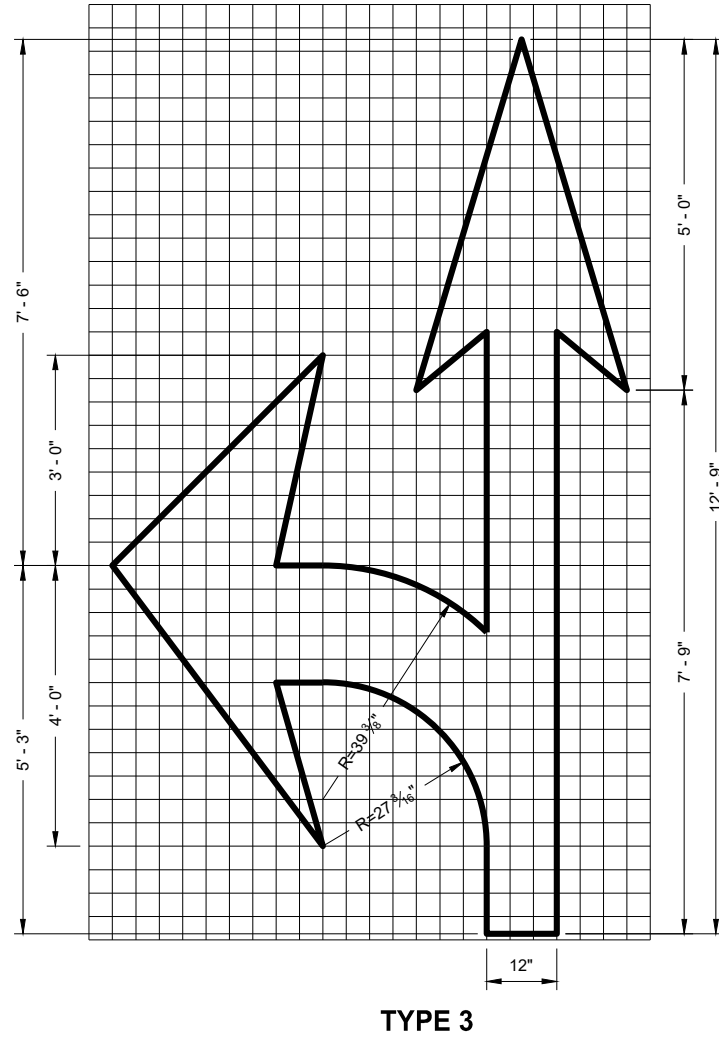
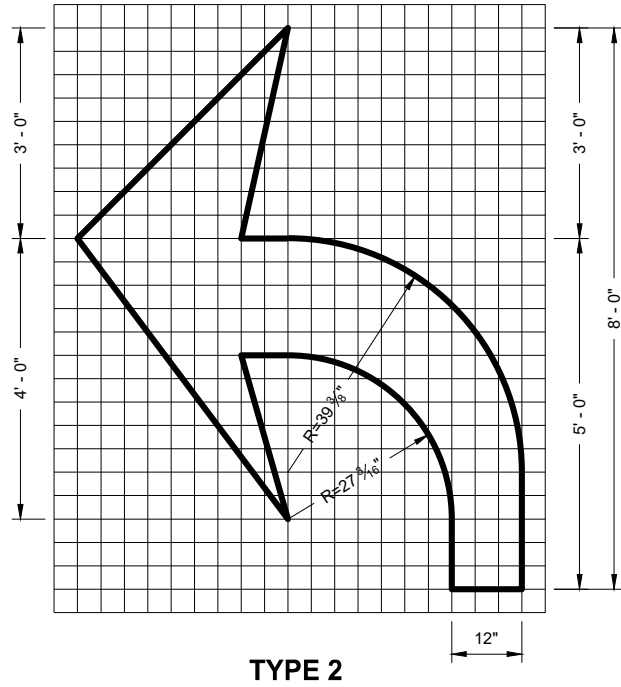
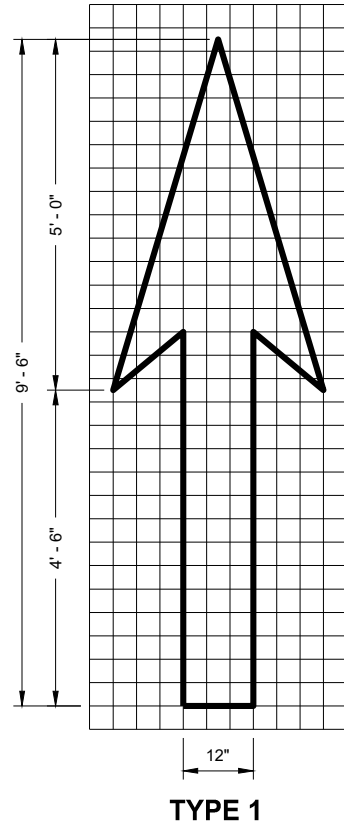
PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

FHWA



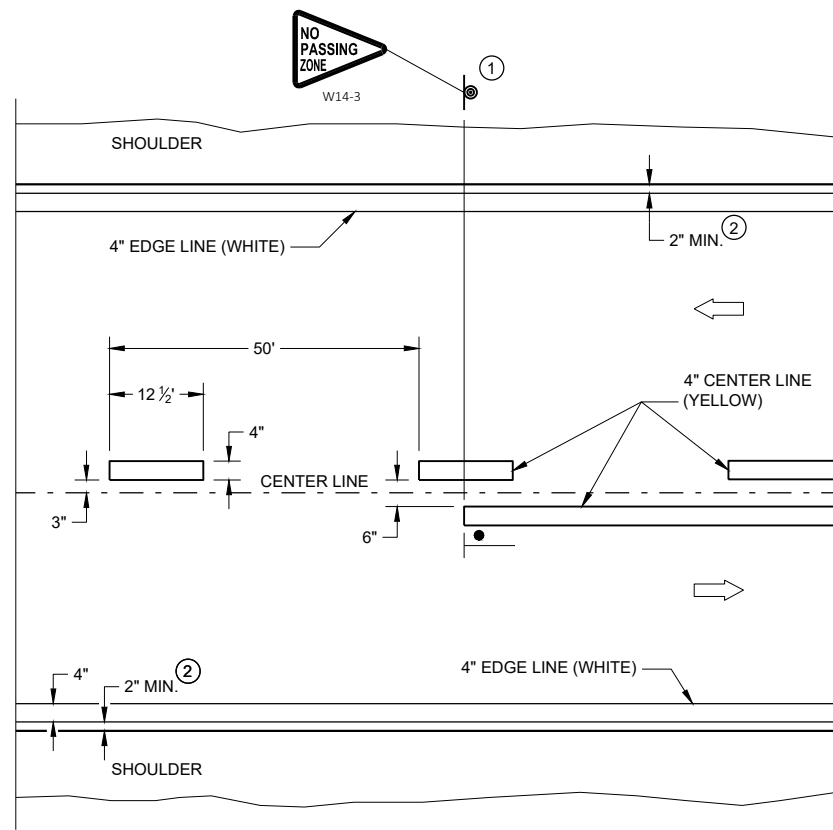
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

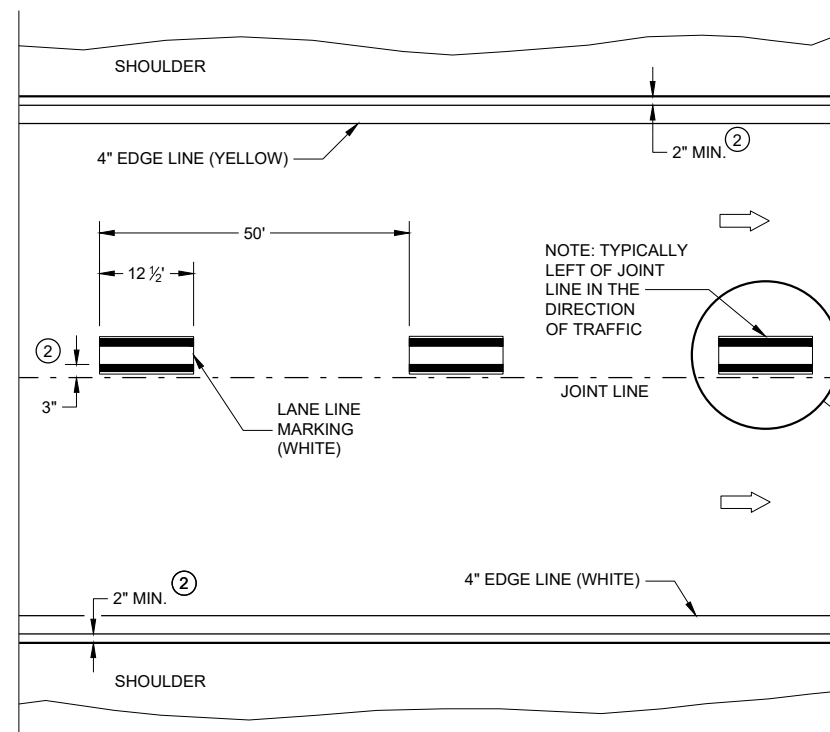
PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE
/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER
FHWA

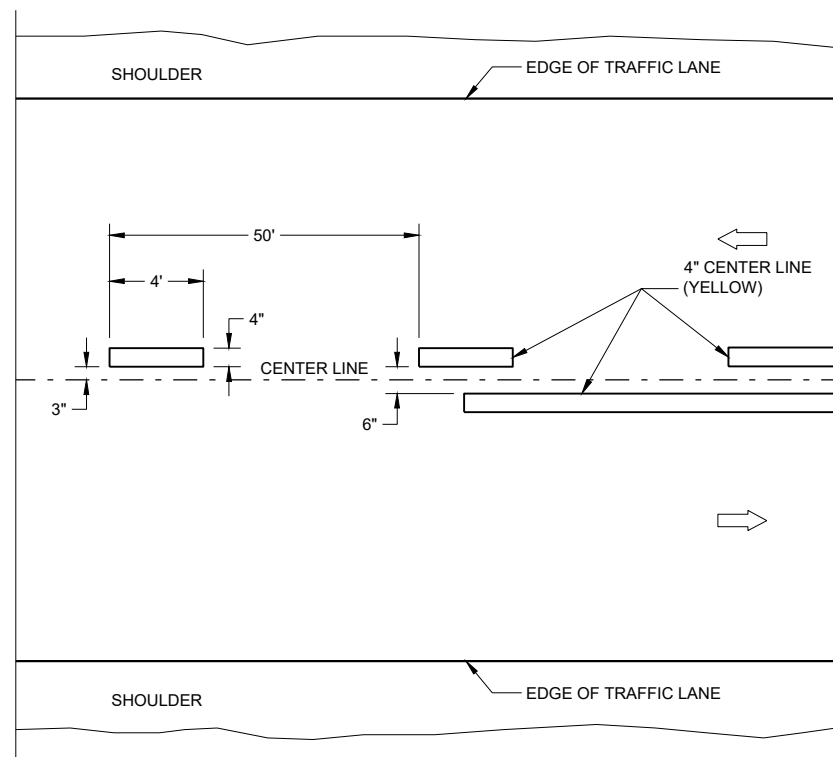


TWO WAY TRAFFIC

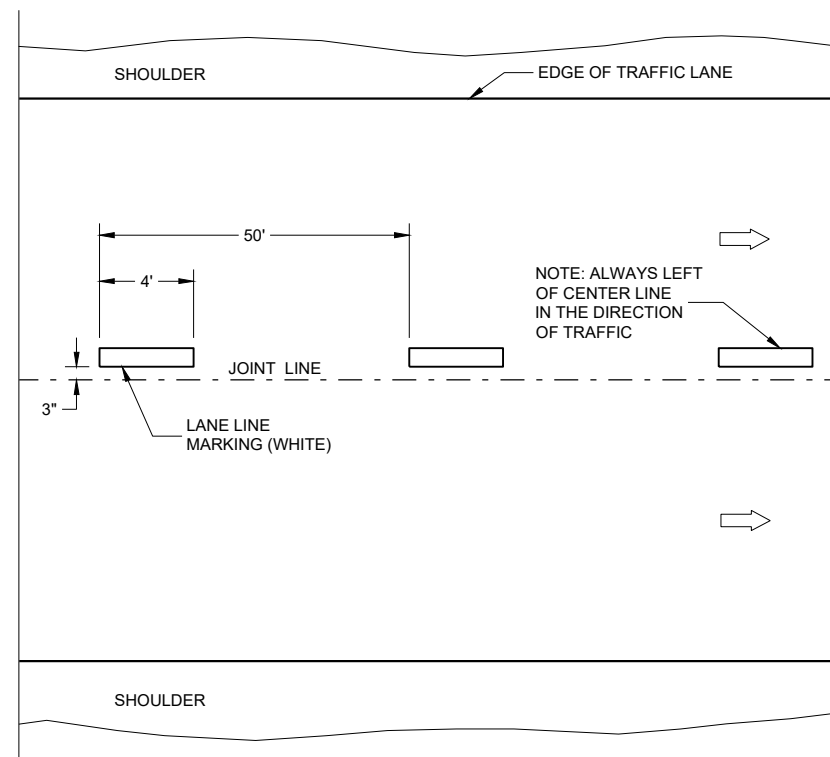


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC




TEMPORARY PAVEMENT MARKING

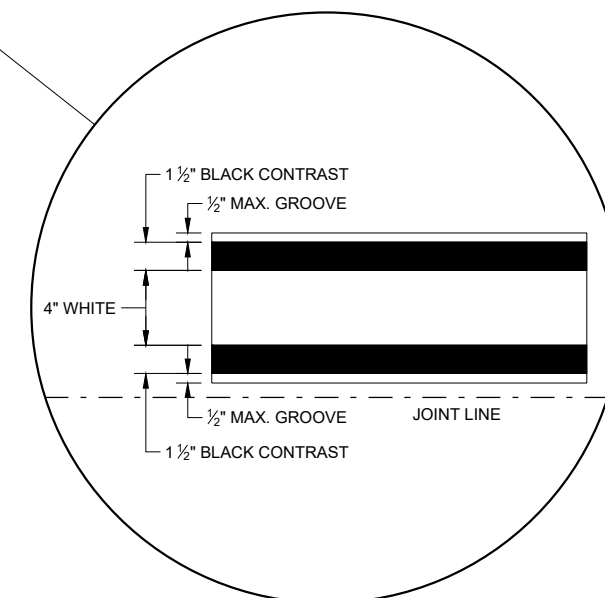
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITH 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
 SIGN ON PERMANENT SUPPORT
 DIRECTION OF TRAFFIC



LONGITUDINAL MARKING (MAINLINE)

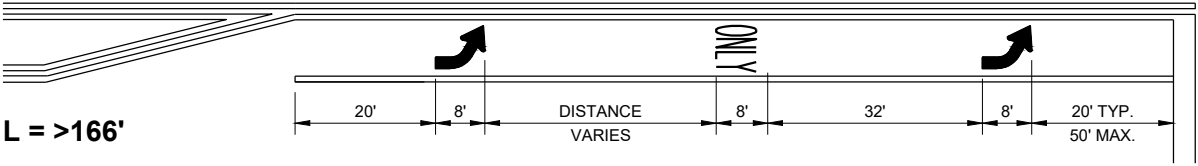
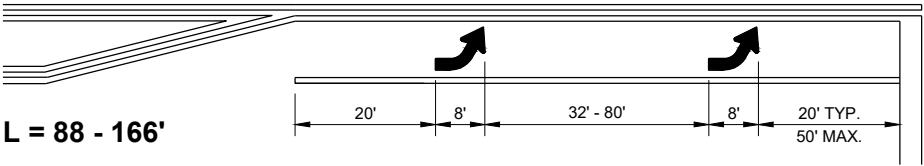
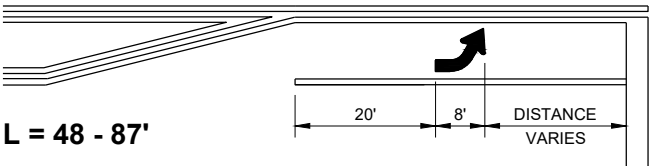
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

/S/ Matthew Rauch
STATEWIDE SIGNING AND MARKING
ENGINEER

TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

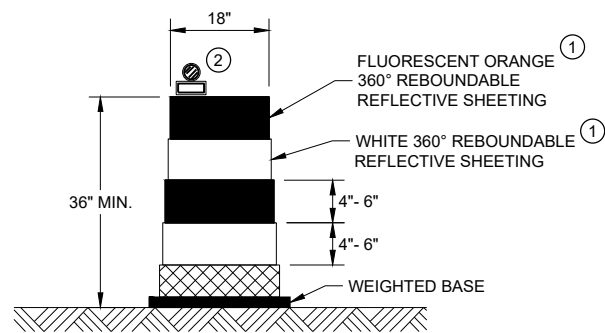
- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

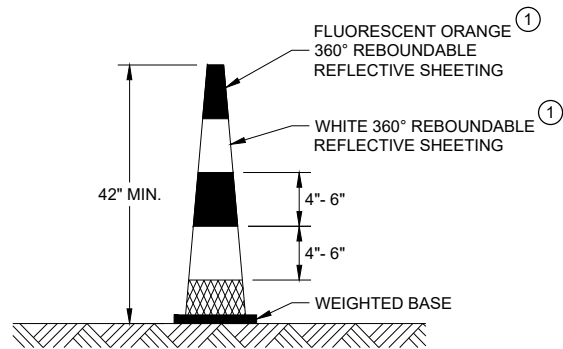
L = LENGTH OF TURN BAY

PAVEMENT MARKING
(TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

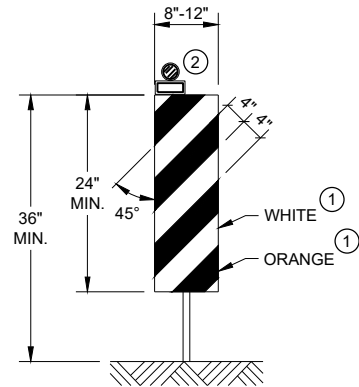


DRUM



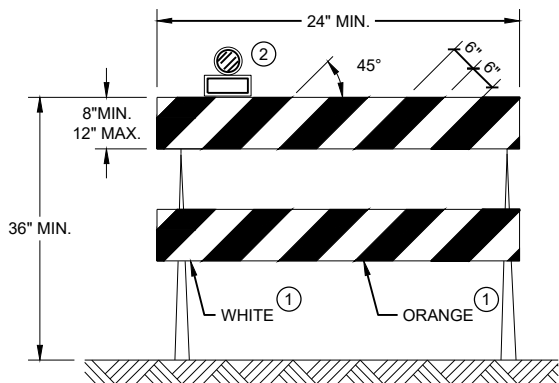
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS



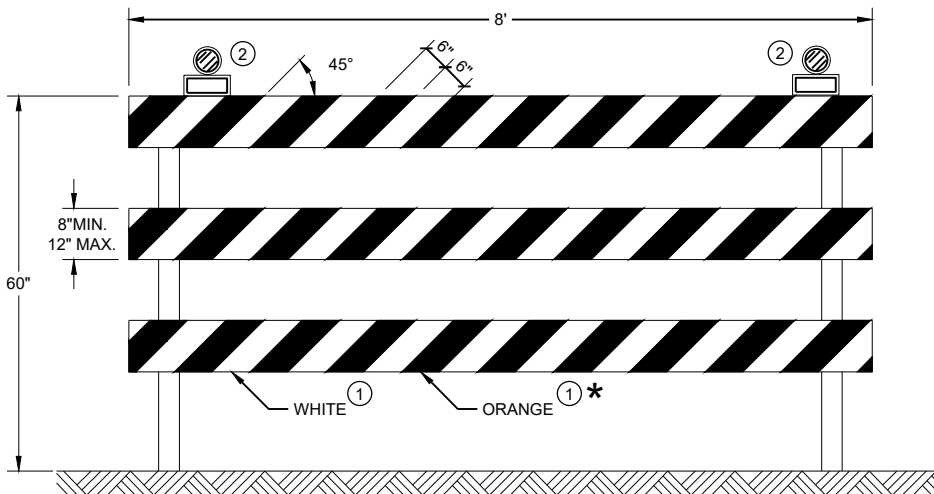
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.






CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

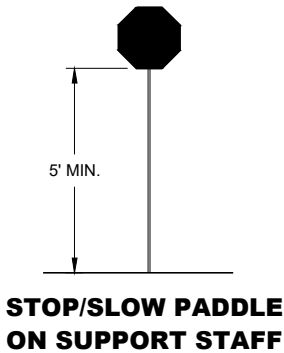
③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

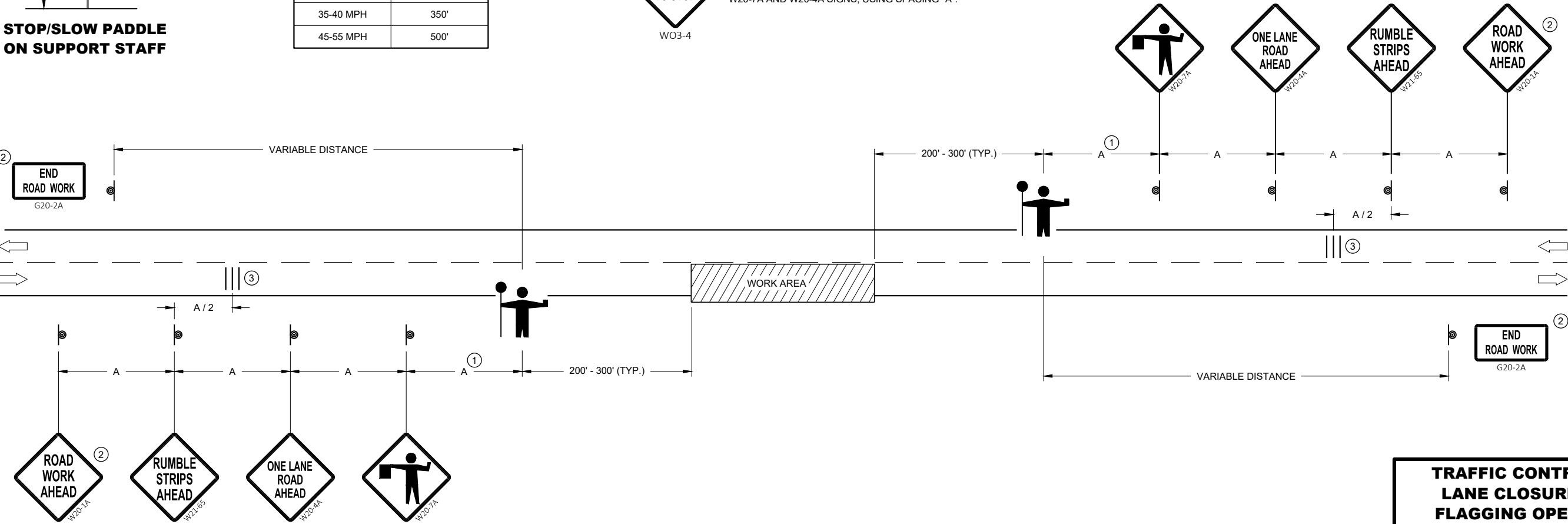


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

| SPEED LIMIT | SPACING "A" |
|-------------|-------------|
| 25-30 MPH | 200' |
| 35-40 MPH | 350' |
| 45-55 MPH | 500' |



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION





**TRAFFIC CONTROL FOR
LANE CLOSURE WITH
FLAGGING OPERATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

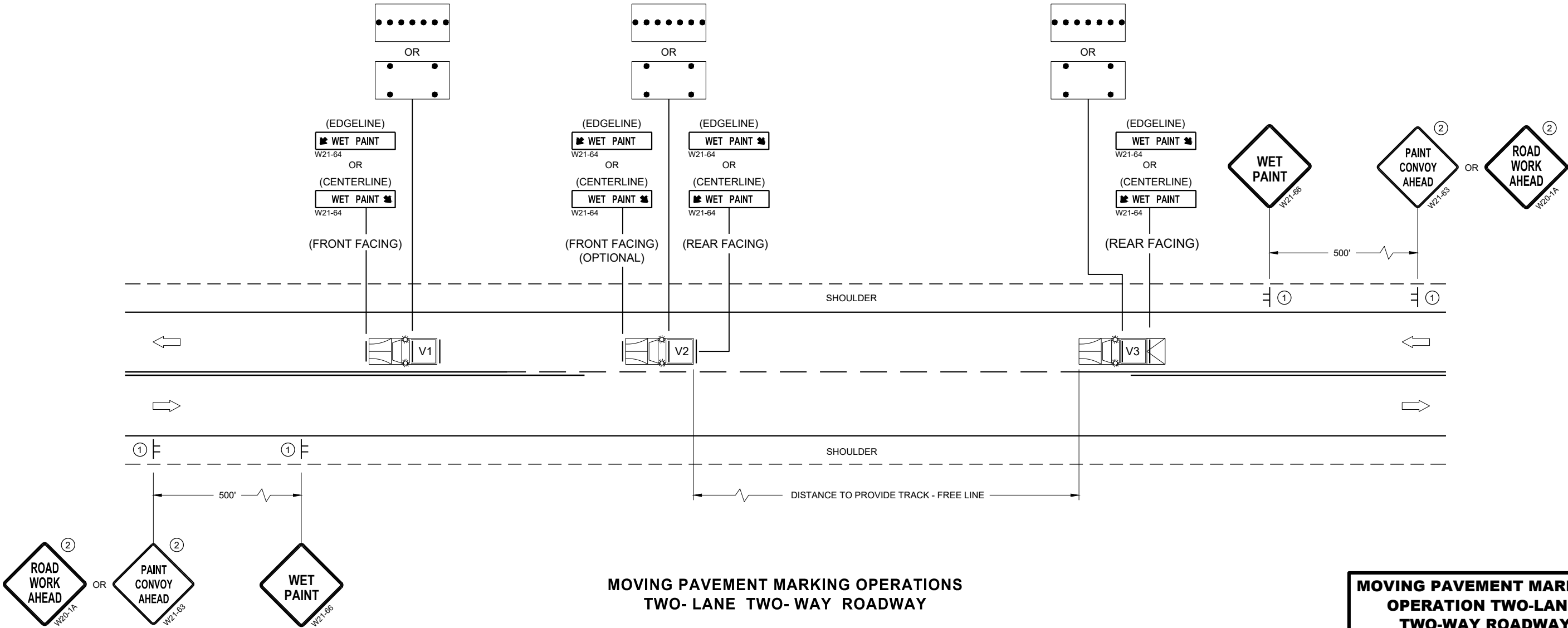
- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.
- DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

- WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.
- CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- CONES SHALL BE A MINIMUM OF 18" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

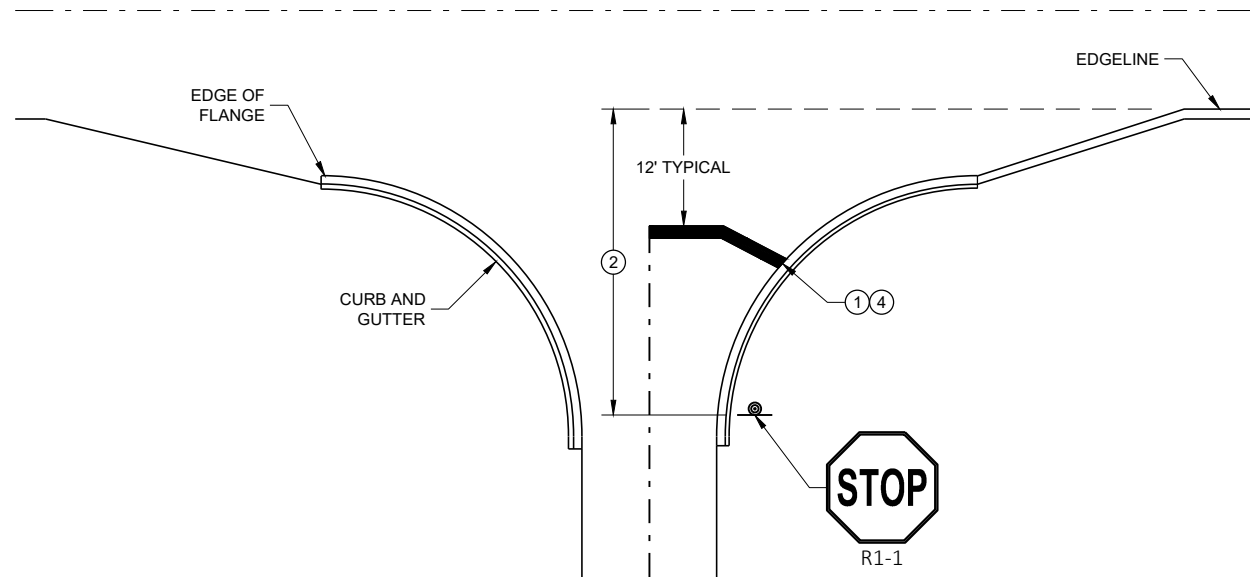


MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY

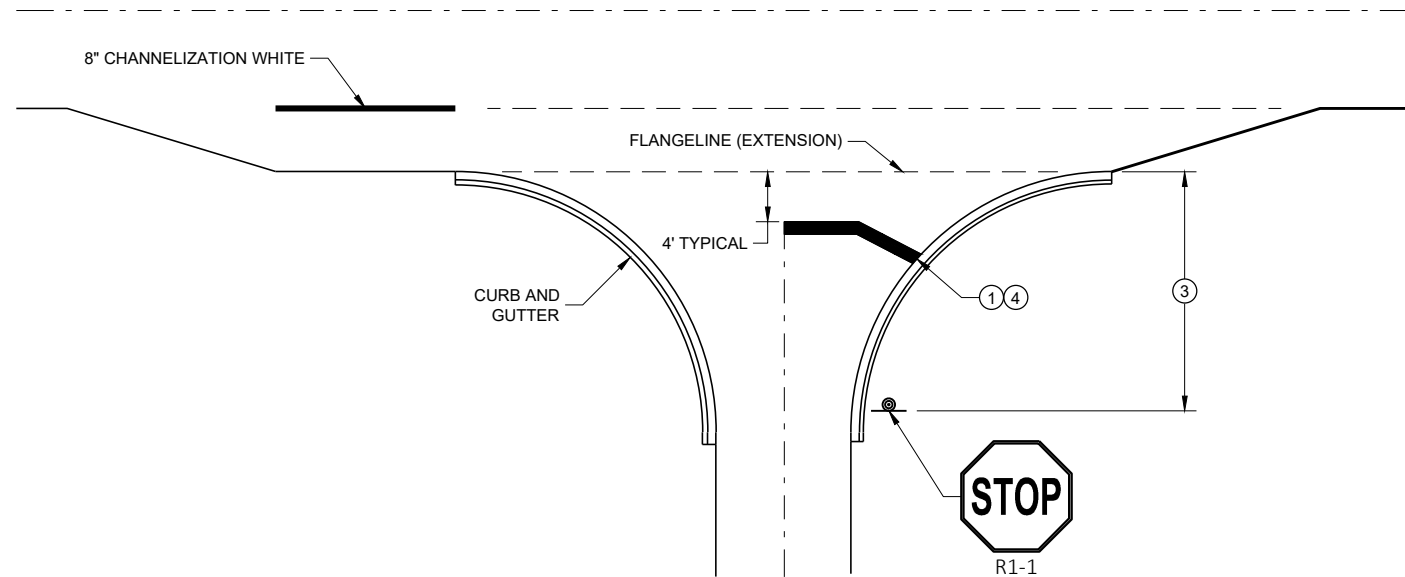
MOVING PAVEMENT MARKING
OPERATION TWO-LANE
TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

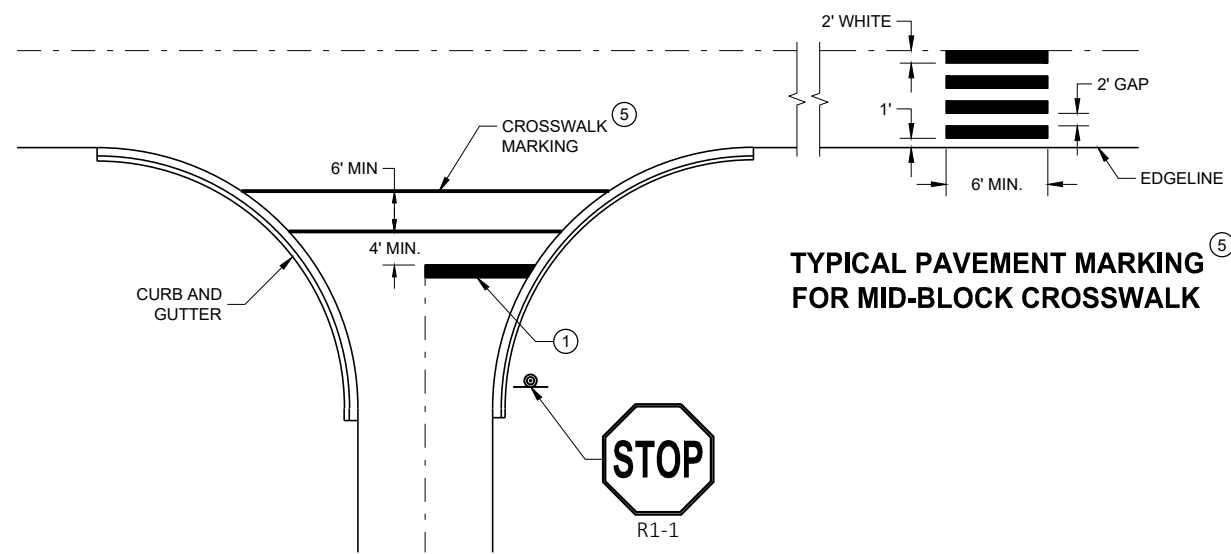
APPROVED
November 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



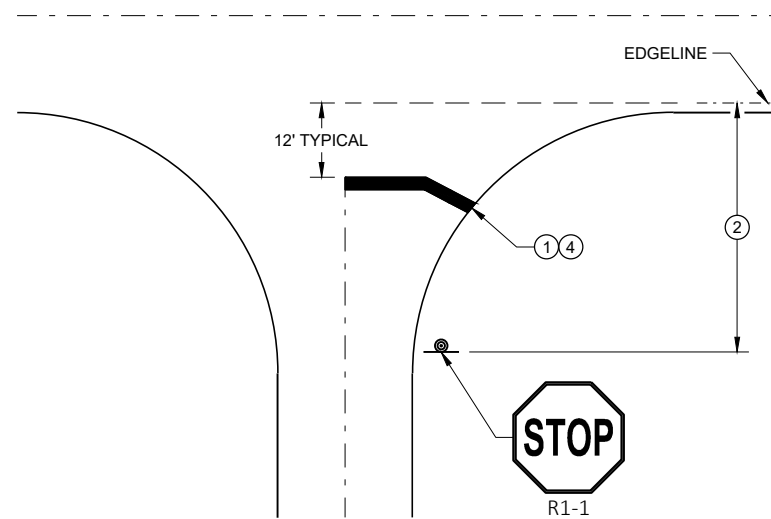
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR
SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

GENERAL NOTES

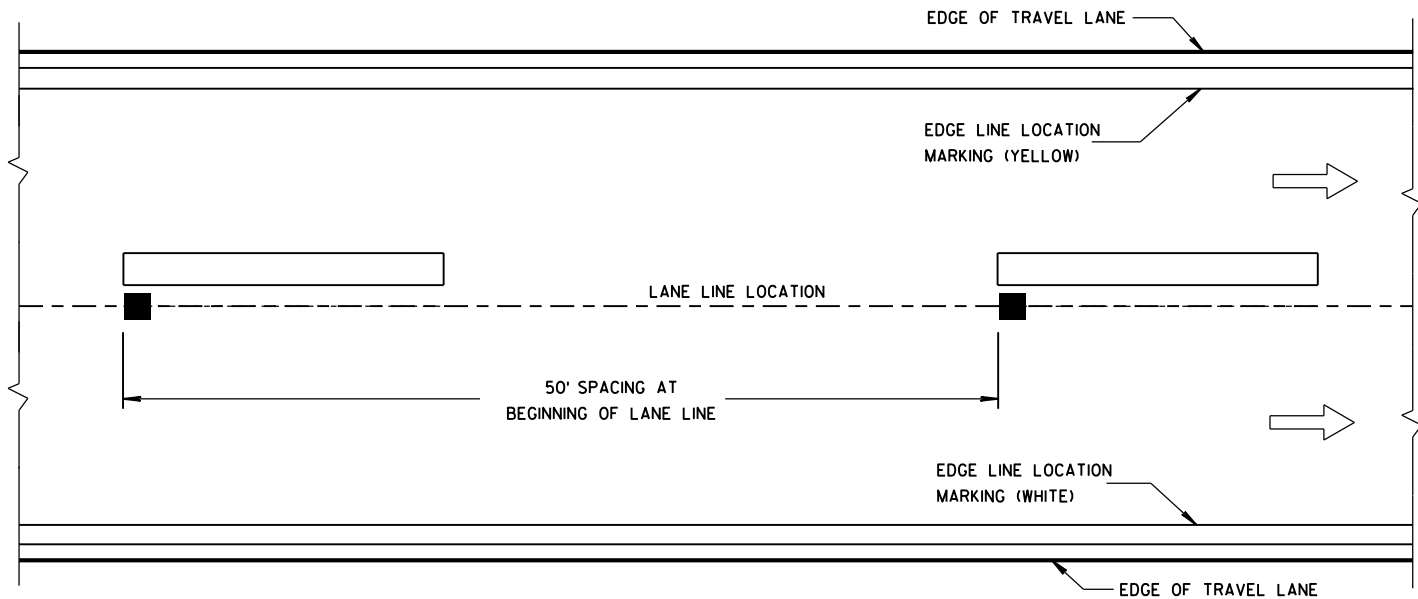
STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGE LINE LOCATION.

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.

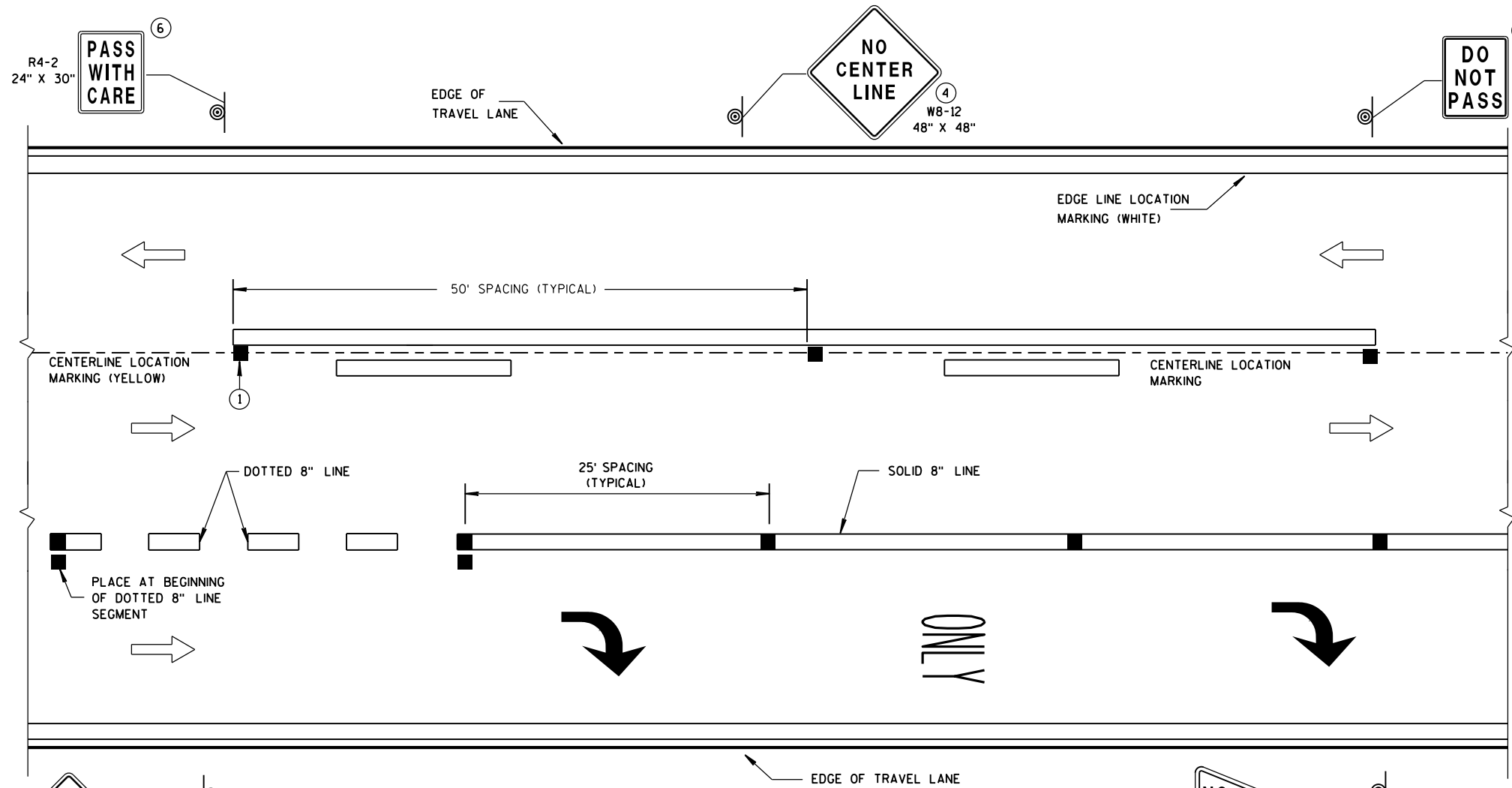
STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER
FHWA



LONGITUDINAL PLACEMENT 4-INCH LANE LINE



LONGITUDINAL PLACEMENT
4-INCH CENTERLINE AND 8-INCH CHANNEL LINE

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

COLOR OF TEMPORARY RAISED PAVEMENT MARKERS, TYPE II SHALL MATCH THE COLOR OF THE MARKING THEY SUPPLEMENT.

PLACEMENT OF TEMPORARY RAISED PAVEMENT MARKERS ON EDGE LINES ARE OPTIONAL. IF PLACED ON EDGE LINES, MAXIMUM SPACING SHALL BE 50 FEET.

PROVIDE SINGLE OR MULTI-COVER TEMPORARY RAISED PAVEMENT MARKERS AS SHOWN ON PLAN.

MARK T's ON PAVEMENT FOR RE-ESTABLISHING NO PASSING ZONES.

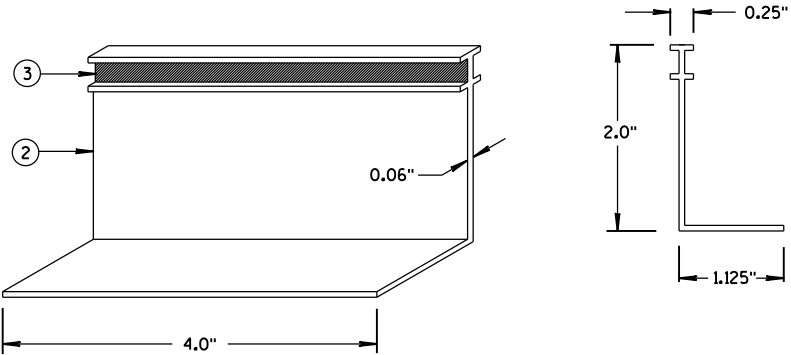
SAME DAY TEMPORARY PAVEMENT MARKING MAY BE USED IN LIEU OF TEMPORARY RAISED PAVEMENT MARKERS, TYPE II.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF TEMPORARY SAME DAY PAVEMENT MARKING IS USED, ENSURE PROPOSED PAVEMENT MARKINGS ARE PLACED IN THE EXACT LOCATIONS AS THE EXISTING MARKINGS, USING A MINIMAL AMOUNT OF TEMPORARY RAISED PAVEMENT MARKERS, TYPE II OR OTHER METHODS AS APPROVED BY THE ENGINEER.

IF ROADWAY IS DETOURED DURING CONSTRUCTION, THE DO NOT PASS, PASS WITH CARE AND NO CENTERLINE SIGNS MAY BE OMITTED, PROVIDED A LIQUID MARKING IS INSTALLED BEFORE THE ROADWAY IS REOPENED TO TRAFFIC.

- 1 FOR DOUBLE SOLID YELLOW, PLACE THE MARKERS BETWEEN THE LINES.
- 2 MARKERS SHALL BE OF POLYURETHANE MATERIAL.
- 3 MARKERS SHALL HAVE A MINIMUM SIZE REFLECTIVE SURFACE OF 4 INCH WIDTH X 0.25 INCH HEIGHT.
- 4 NO CENTER LINE SIGNS SHALL BE PLACED AT THE BEGINNING OF PROJECT, AT TWO-MILE INTERVALS AND AFTER STATE AND COUNTY HIGHWAY INTERSECTIONS.
- 5 DO NOT PASS SIGNS SHALL BE INSTALLED AT THE BEGINNING OF NO PASSING ZONES. ADDITIONAL DO NOT PASS SIGNS SHALL BE INSTALLED AT ONE MILE INTERVALS AND AFTER STATE AND COUNTY HIGHWAY INTERSECTIONS WITHIN THE NO PASSING ZONE.
- 6 PASS WITH CARE SIGNS SHALL BE PLACED AT THE DOWNSTREAM END OF NO PASSING ZONES.



ISOMETRIC VIEW SIDE VIEW

TEMPORARY RAISED
PAVEMENT MARKER, TYPE II

LEGEND

- TEMPORARY RAISED PAVEMENT MARKER, TYPE II
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- DIRECTION OF TRAVEL

STANDARD APPLICATION FOR
TEMPORARY RAISED PAVEMENT
MARKERS, TYPE II

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6-2017 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

LEGEND





-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

TABLE A

| SHOULDER TAPER LENGTH (FEET) | | | | | BUFFER SPACE (FEET) |
|------------------------------|----|-----|-----|-----|---------------------|
| S \ W | 4 | 6 | 8 | 10 | |
| 30 | 20 | 30 | 40 | 50 | 200 |
| 35 | 30 | 45 | 55 | 70 | 250 |
| 40 | 40 | 55 | 75 | 90 | 305 |
| 45 | 60 | 90 | 120 | 150 | 360 |
| 50 | 70 | 100 | 135 | 170 | 425 |
| 55 | 75 | 110 | 150 | 185 | 495 |

W = SHOULDER WIDTH (FEET
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH

L= WS AT 45 MPH OR GREATER
L = WS² / 60 AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = 1/3L

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

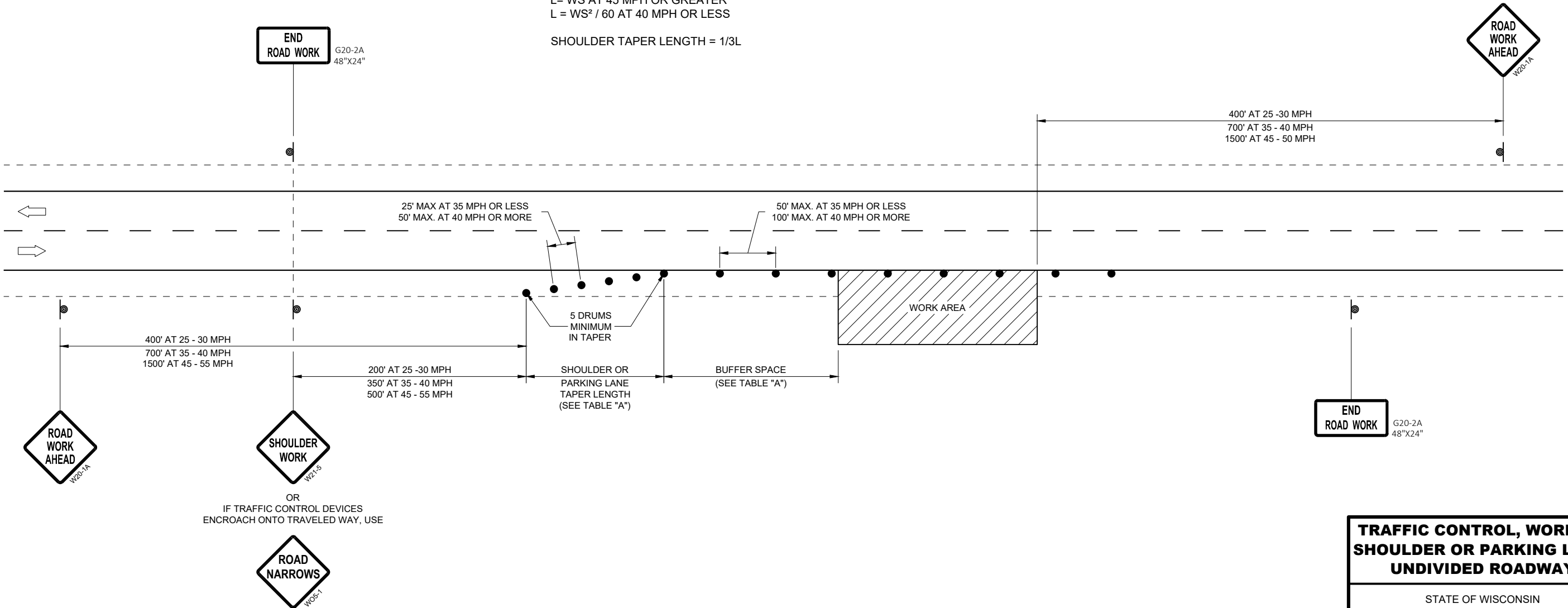
W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY RESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

6

SDD 15D28 - 03

SDD 15D28 - 03



TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

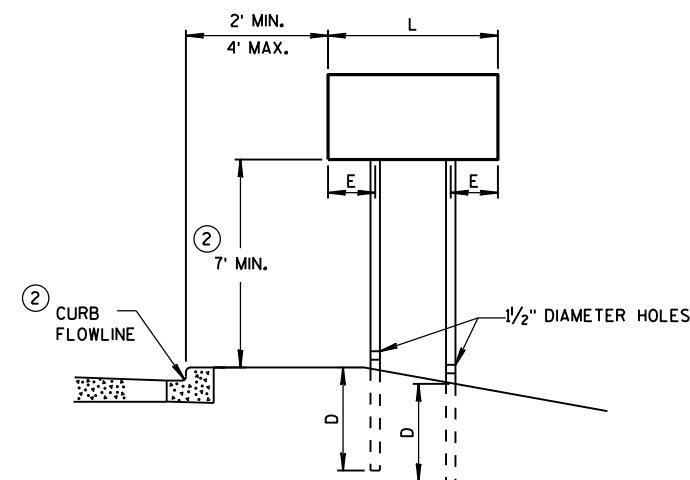
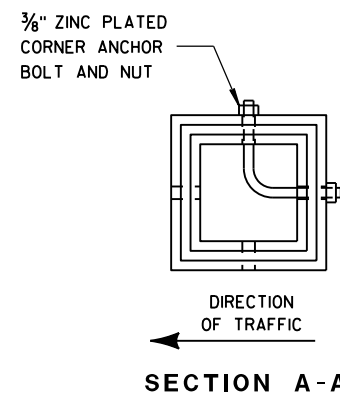
APPROVED
August 2019
DATE /S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER
FHWA



TUBULAR STEEL POSTS

| AREA OF SIGN INSTALLATION (SQ. FT.) | NUMBER OF REQUIRED TUBULAR STEEL POSTS |
|------------------------------------------------|----------------------------------------------|
| 9 OR LESS | 1 |
| GREATER THAN 9 LESS THAN OR EQUAL TO 18 | 2 |
| GREATER THAN 18 LESS THAN OR EQUAL TO 27 | 3 |

SIGNS LARGER THAN 27 SQ.FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

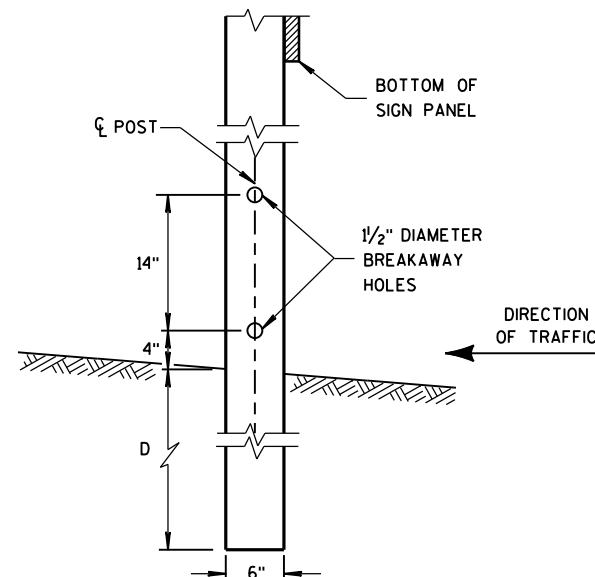


URBAN AREA

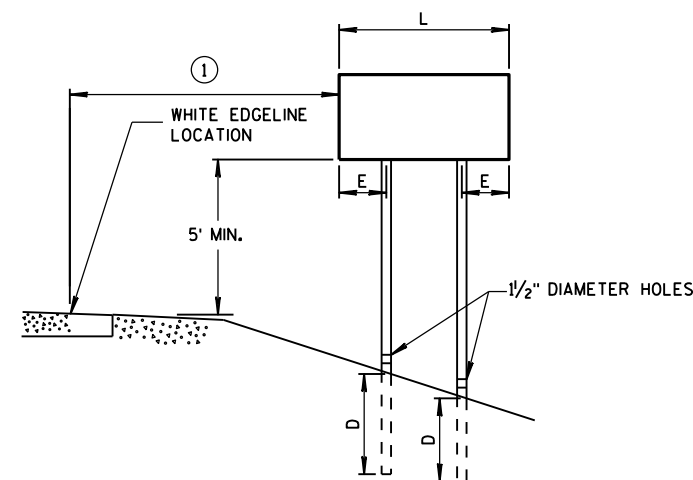
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST
EMBEDMENT DEPTH

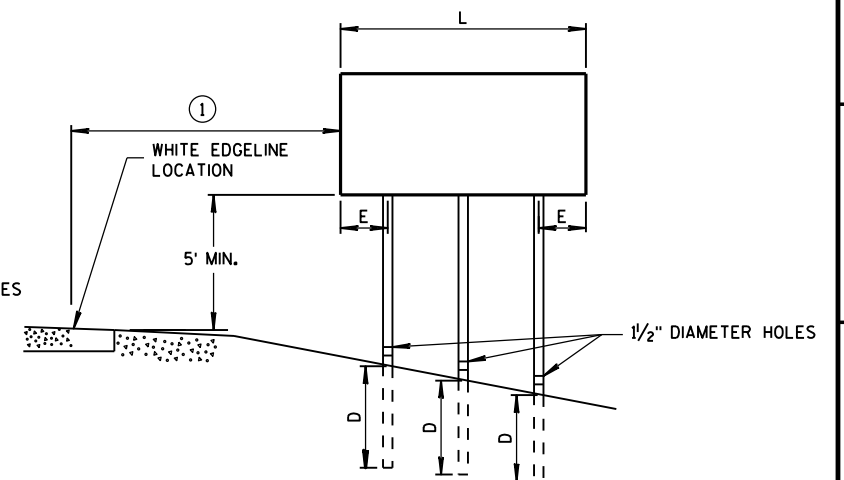
| | |
|-------------------------------------------|------------|
| AREA OF SIGN INSTALLATION (SQ. FT.) | D (MIN) |
| 20 OR LESS | 4' |
| GREATER THAN 20 | 5' |



4"x6" WOOD POST MODIFICATION



RURAL AREA



GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

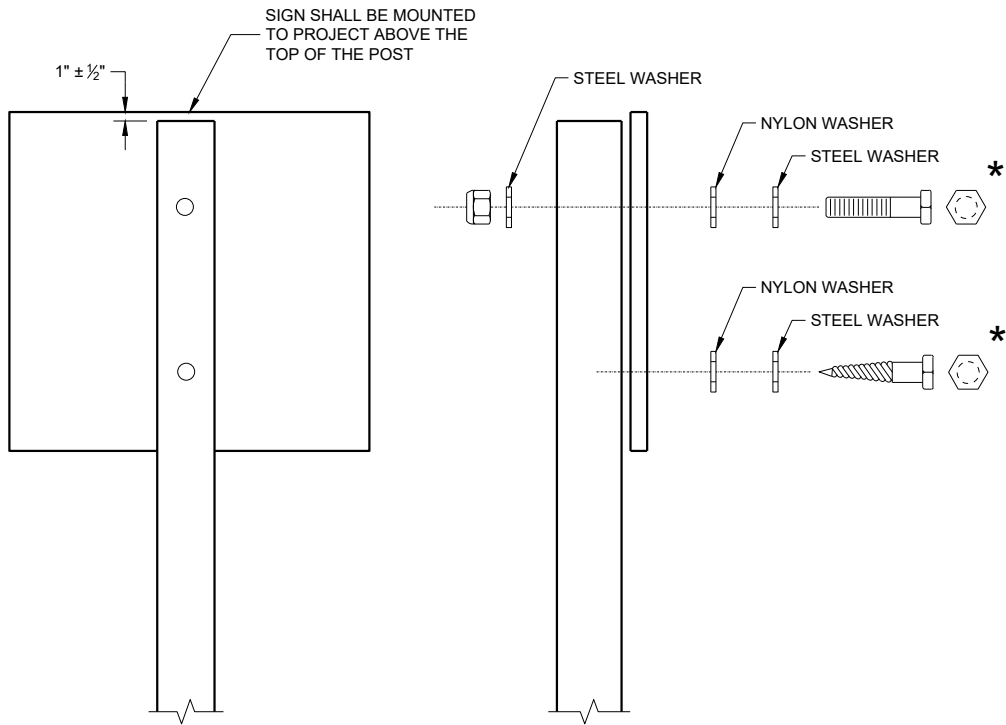
4" X 6" WOOD POST

| POST SPACING REQUIREMENTS | | NUMBER OF WOOD POSTS REQUIRED |
|-----------------------------------------|-----|-------------------------------------|
| L | E | |
| 48" OR LESS AND LESS THAN 20 SQ. FT. | - | 1 |
| LESS THAN 60" | 12" | 2 |
| 60" TO 120" | L/5 | 2 |
| GREATER THAN 120" LESS THAN 168" | 12" | 3 |
| 168" AND GREATER | 12" | 4 |

SEE NOTE (3)

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS
SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM
DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM
DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH
SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED
COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POST (4" x 6")
LAG SCREWS - 3/8" x 3"
MACHINE BOLTS - 5/16" x 6 1/2" OR 7" LENGTH W/NUTS

SQUARE STEEL POST (2" x 2")
MACHINE BOLTS - 3/8" x 3 1/4" LENGTH W/NUTS
RIVETS - 3/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM
BODY/MANDREL O.D. FLANGE 0.720 - 0.765 INCH,
GRIP RANGE 0.042 - 0.375 INCH

WASHERS (ALL POSTS) -
1 1/4" O.D. x 3/8" I.D. x 1/16" STEEL
1 1/4" O.D. x 3/8" I.D. x 0.080 NYLON

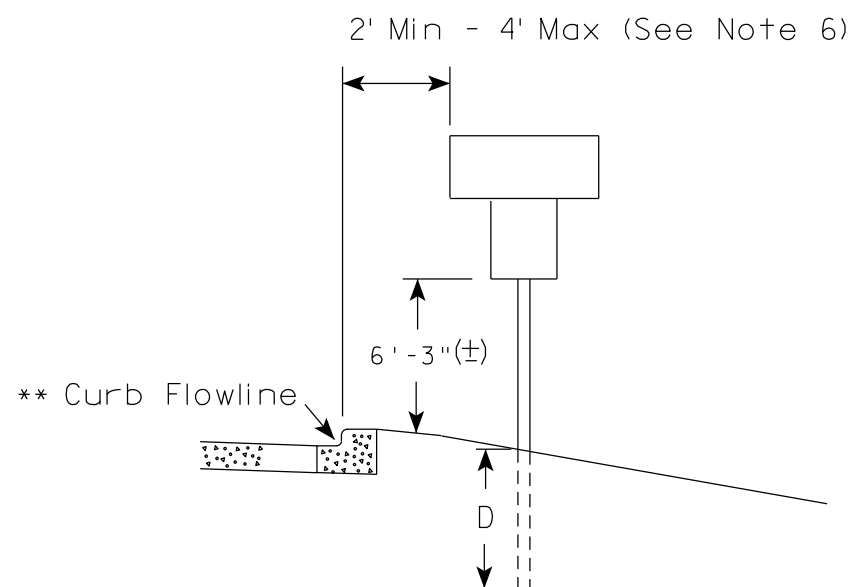
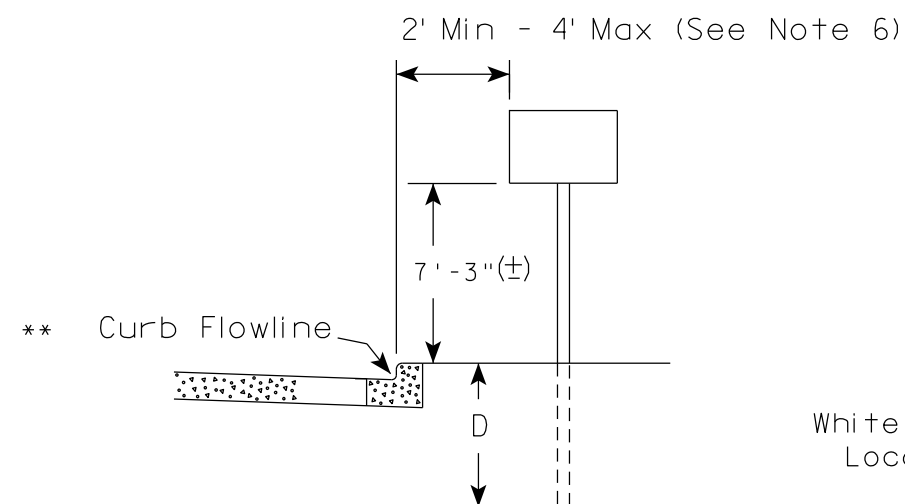
* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION
PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM
SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH
THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER
THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS
TO POSTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

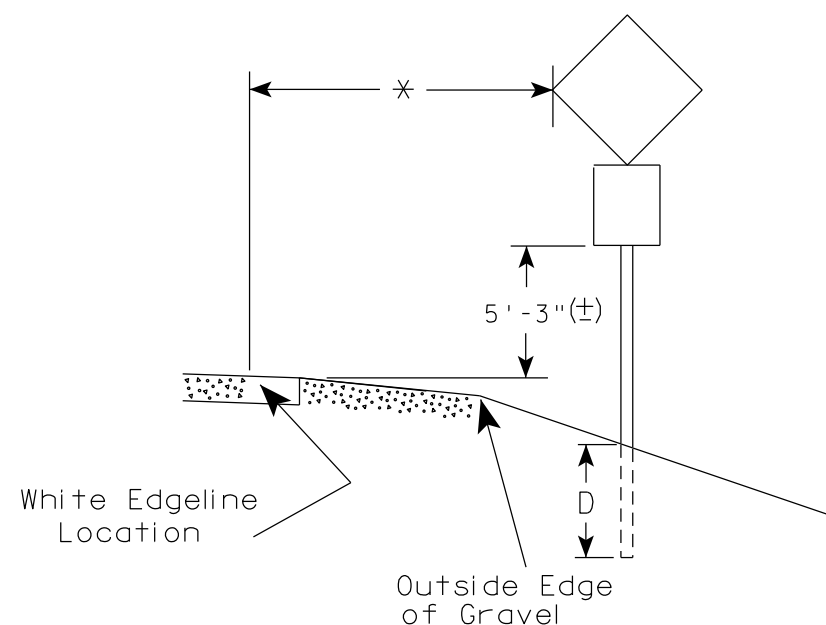
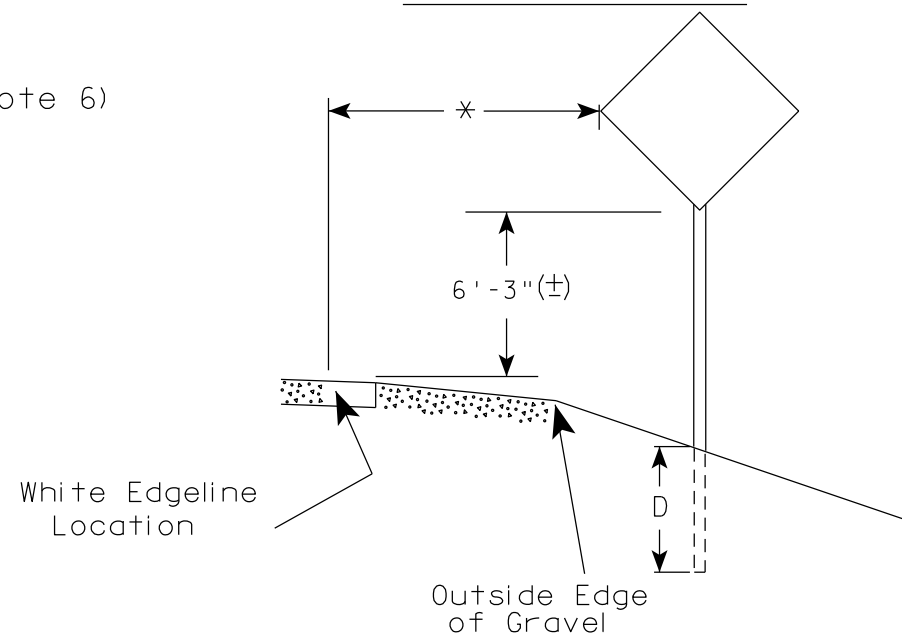
APPROVED
June 2017 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

URBAN AREA



** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

RURAL AREA (See Note 2)



* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|------------------------------------------|--------------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

GENERAL NOTES

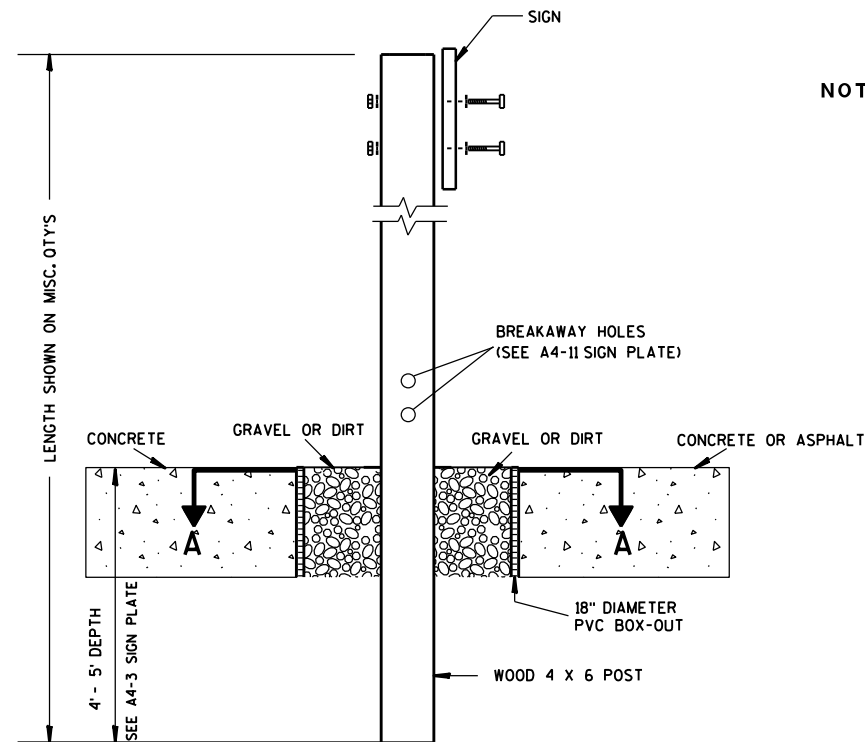
1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

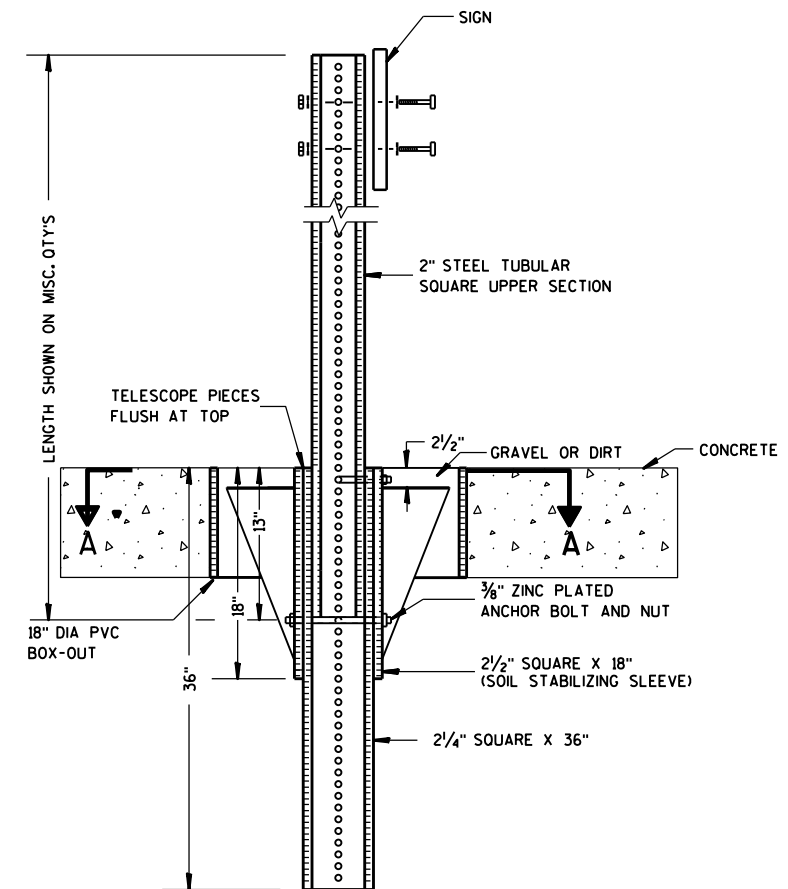
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

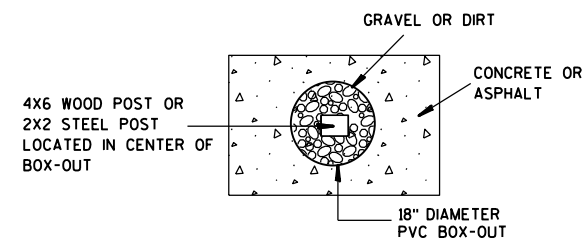
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

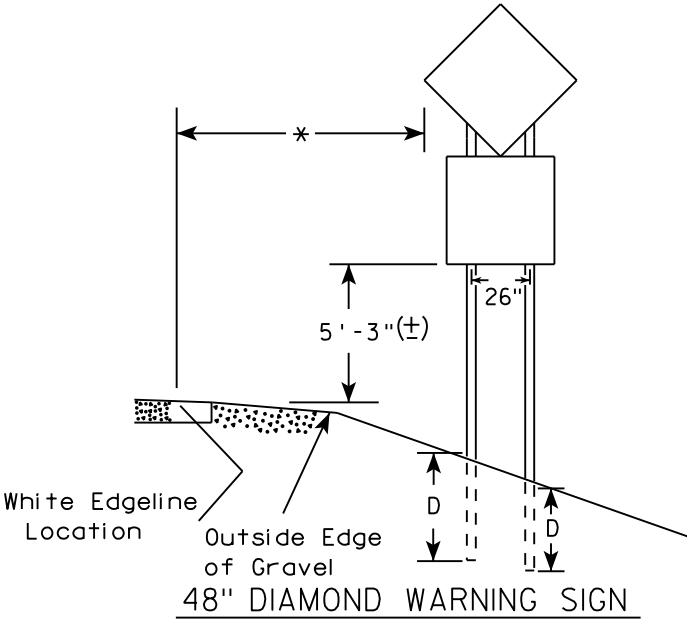
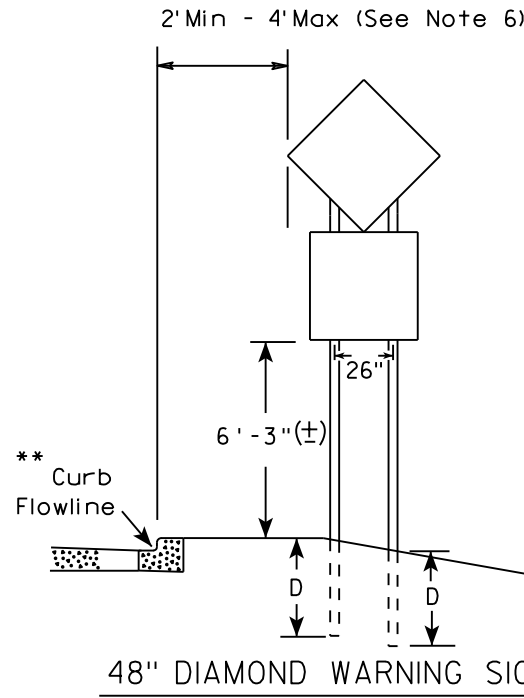
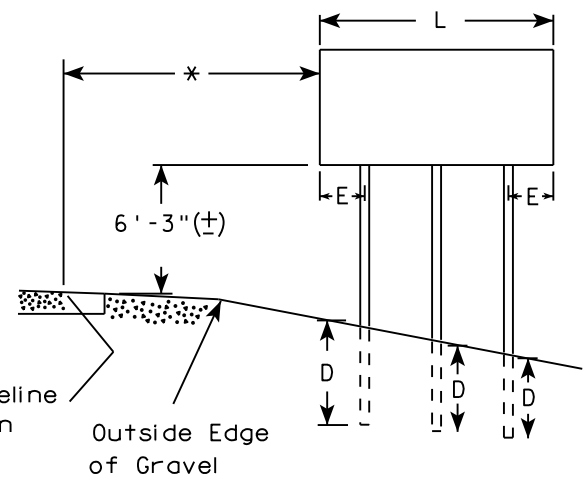
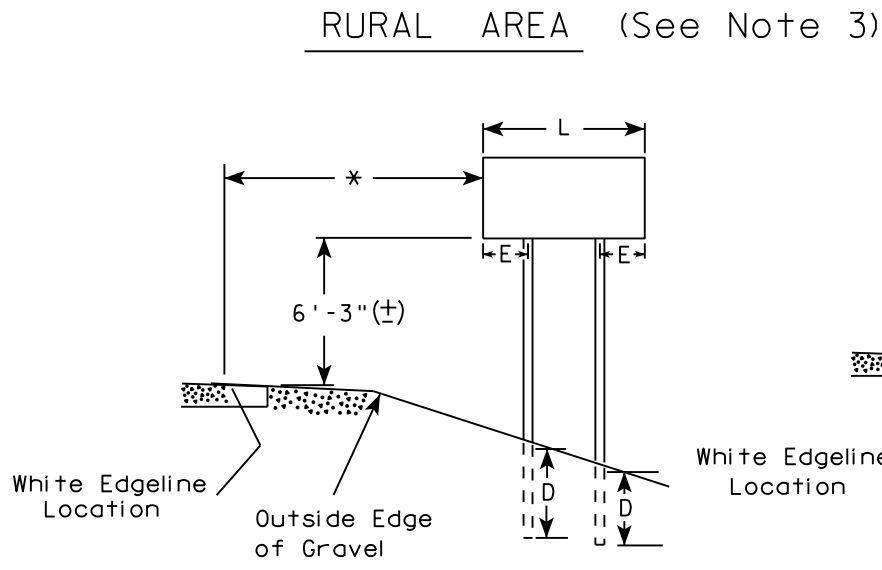
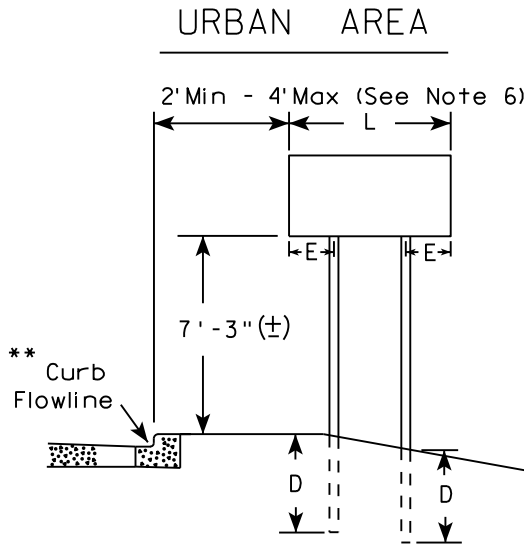
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



| SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED) | |
|-------------------------------------------------------|-----|
| L | E |
| Greater than 48" Less than 60" | 12" |
| 60" to 108" | L/5 |

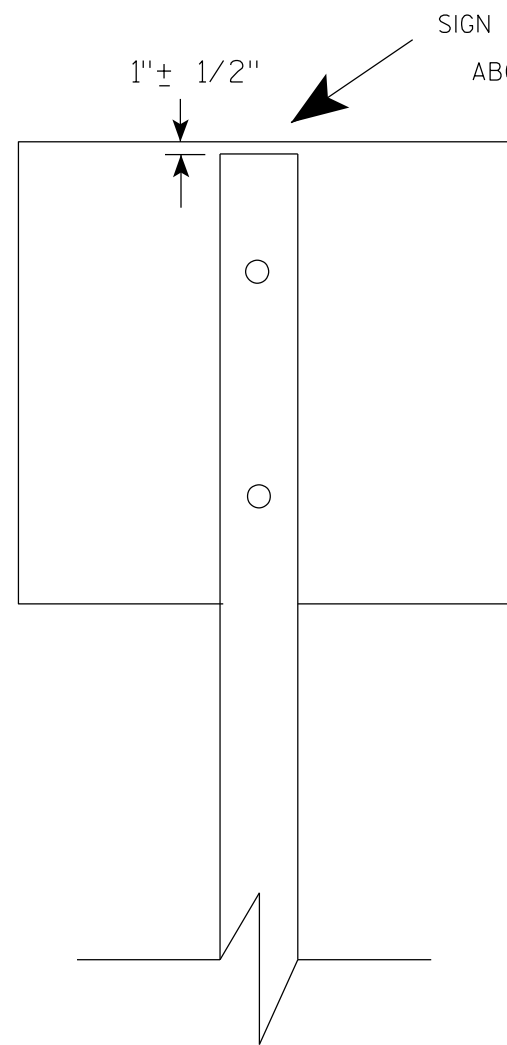
| SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED) | |
|---------------------------------------------------------|-----|
| L | E |
| Greater than 108" to 144" | 12" |

| POST EMBEDMENT DEPTH | |
|---------------------------------------------|--------------|
| Area of Sign Installation (Sq. Ft.) | D (Min) |
| 20 or Less | 4' |
| Greater than 20 | 5' |

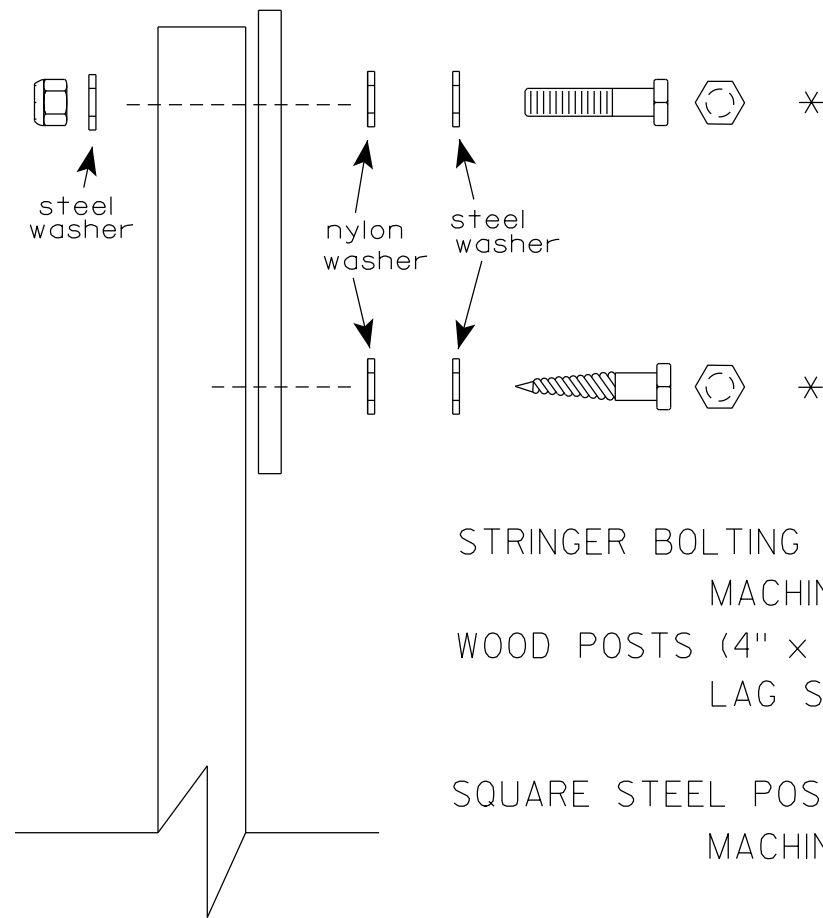
| | |
|---------------------------------------------------------------|-------------------------------------------------------|
| TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS | |
| WISCONSIN DEPT OF TRANSPORTATION | |
| APPROVED | <i>Matthew R. Rauch</i> for State Traffic Engineer |
| DATE 8/21/17 | PLATE NO. A4-4.15 |

- GENERAL NOTES
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. J-Assemblies are considered to be one sign for mounting height.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- *** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.



SIGN SHALL BE MOUNTED TO PROJECT
ABOVE THE TOP OF THE POST



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.


ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch
For State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**

4" x 10" x 10 GA. — 
STEEL PLATE (CUT
AS SHOWN) WELDED
TO ALL FOUR CORNERS
OF TELESPAR TUBE

**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**

2 1/2" TELESPAR TUBE

4"

2 1/2"

10"

3 1/2"

19"

4" x 10" x 10 GA. STEEL PLATE (CUT AS SHOWN) WELDED TO ALL FOUR CORNERS OF TELESPAR TUBE

LENGTH SHOWN ON MISC. QTY'S
 18" DIA SCHEDULE 40 PVC BOX-OUT
 TELESCOPE PIECES FLUSH AT TOP
 36"
 18"
 13"
 2 1/2"
 2 1/4" SQUARE X 36"
 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)
 3/8" ZINC PLATED ANCHOR BOLT AND NUT
 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT
 ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES
 2" STEEL TUBULAR SQUARE UPPER SECTION
 SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL
 SIGN
 2 1/2" GRAVEL OR DIRT

LENGTH SHOWN ON MISC. QTY'S

TELESCOPE PIECES FLUSH AT TOP

2" STEEL TUBULAR SQUARE UPPER SECTION

ALL HOLES $\frac{7}{16}$ " SPACED 1" C-C ALL FOUR SIDES

$\frac{3}{8}$ " ZINC PLATED CORNER ANCHOR BOLT AND NUT

1"

$\frac{3}{8}$ " ZINC PLATED ANCHOR BOLT AND NUT

2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)

2 1/4" SQUARE X 36"

SIGN

SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL

18"

12"

36"


A

B

C

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT

DIRECTION
OF TRAFFIC



SECTION A-A

| Area of Sign Installation (Sq. Ft.) | Number of Required Posts |
|------------------------------------------|--------------------------|
| 9 or less | 1 |
| Greater than 9 less than or equal to 18 | 2 |
| Greater than 18 less than or equal to 27 | 3 |

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthieu R. Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

| | | | | |
|-------------|------|---------|-----------|---|
| PROJECT NO: | HWY: | COUNTY: | SHEET NO: | E |
|-------------|------|---------|-----------|---|

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PLOT DATE : 05-FEB-2015 17:09

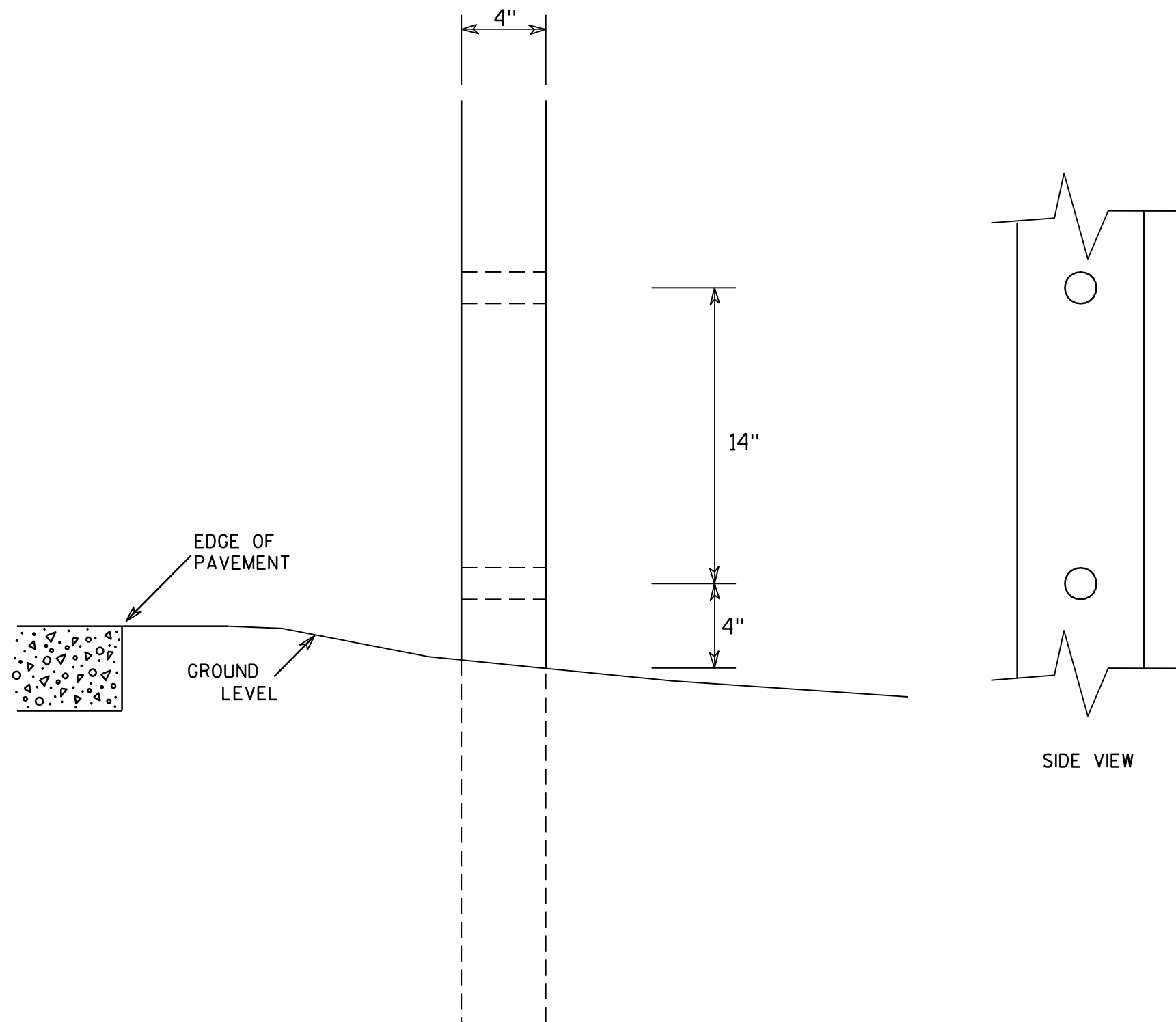
PLOT BY : mscs_ja

PLOT NAME :

PLOT SCALE : 13.659812:1.000000

WISDOT/CADDS SHEET 42

7



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

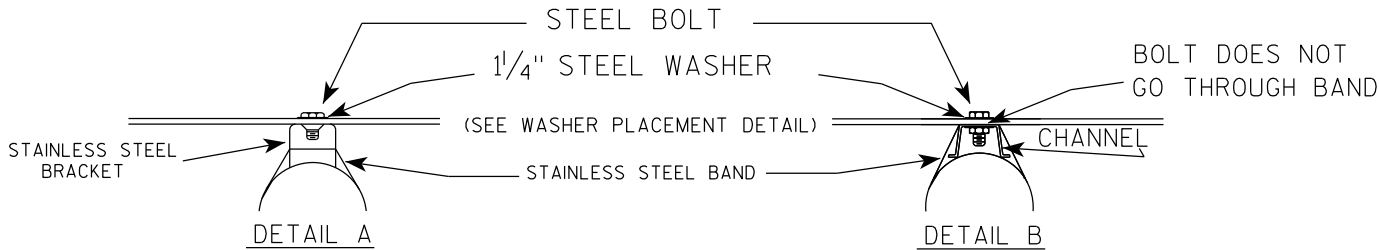
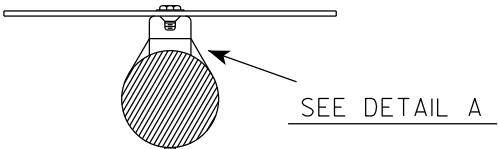
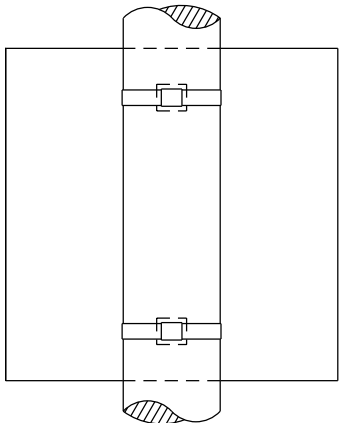
COUNTY:

SHEET NO:

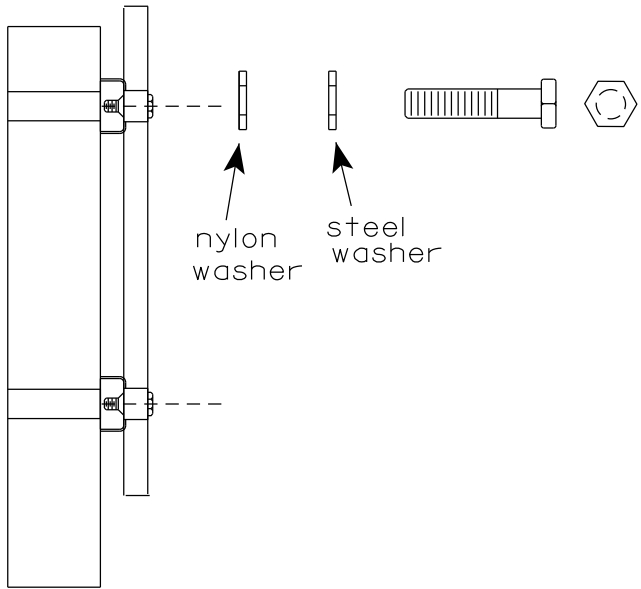
E

BANDING

SINGLE SIGN



WASHER PLACEMENT

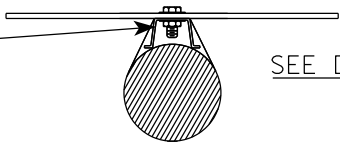
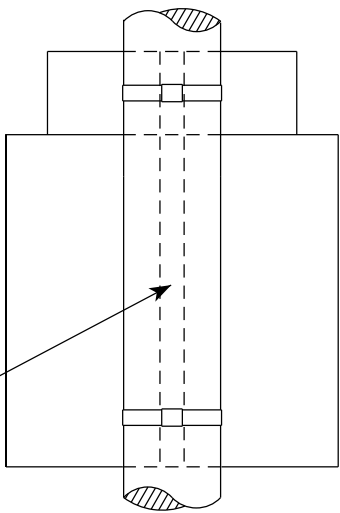


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY

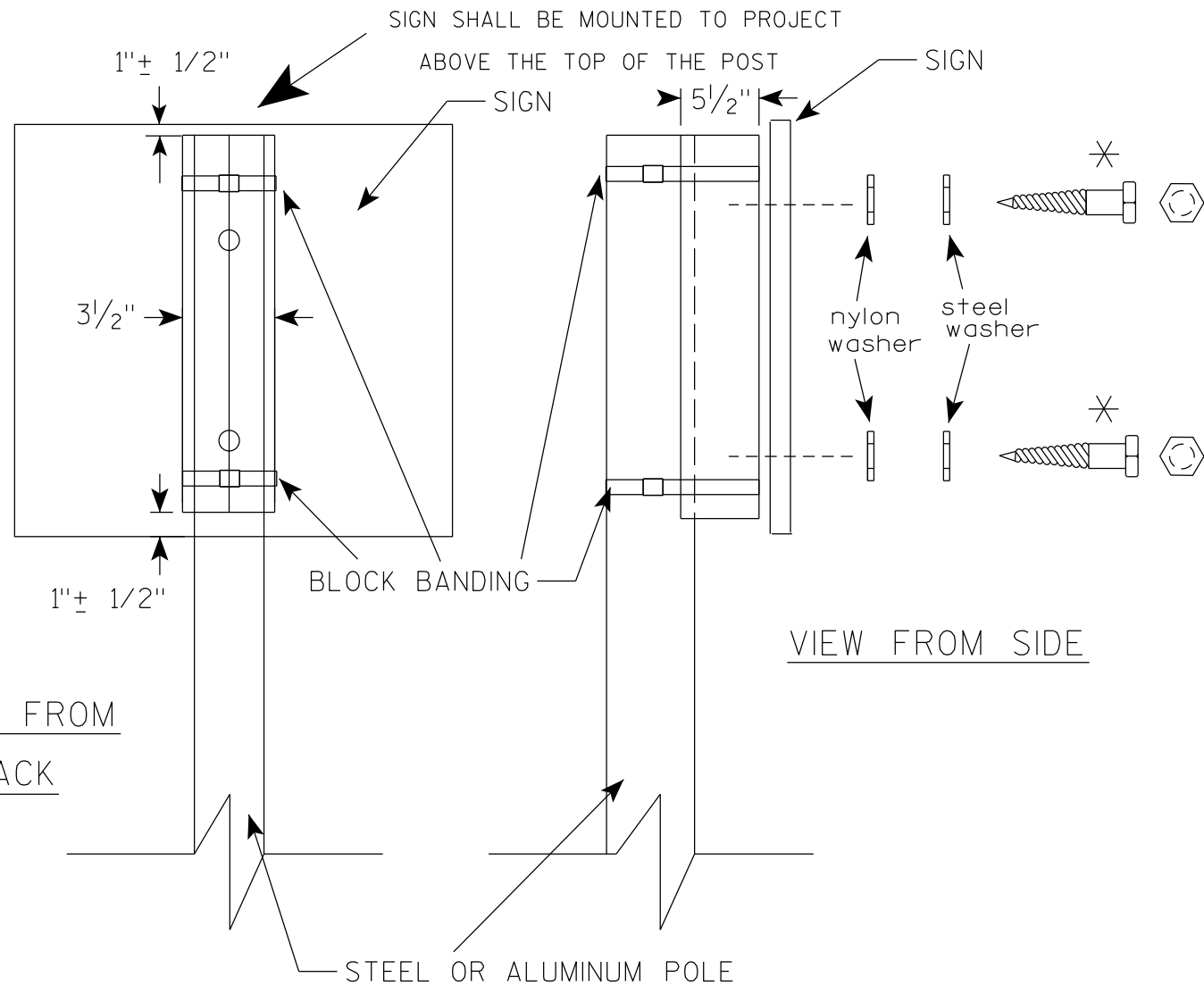


STANDARD SIGN
SIGN BANDING DETAILS

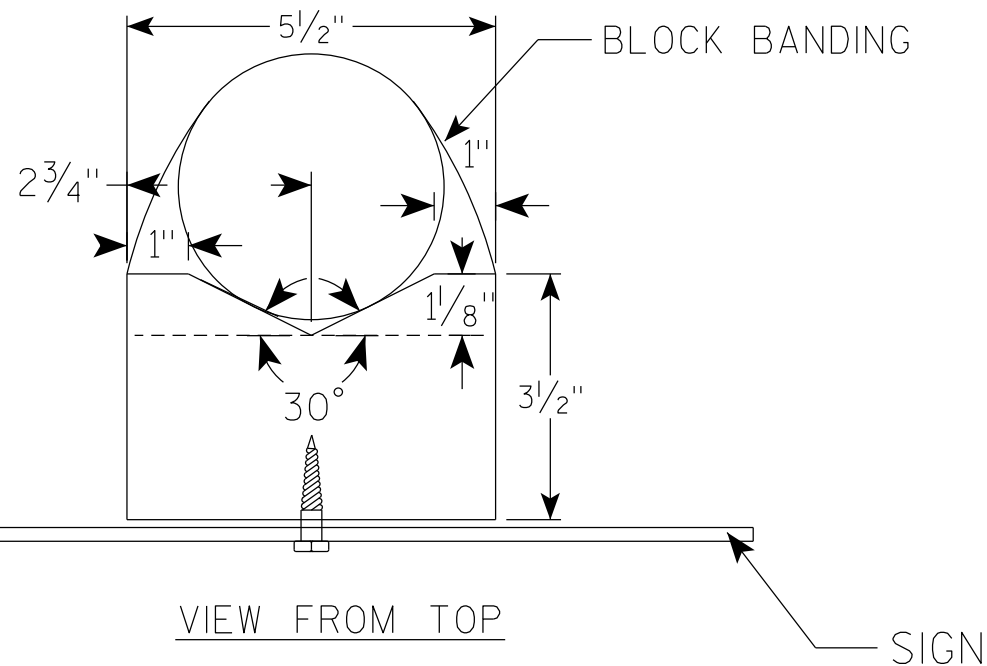
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/10/19 PLATE NO. A5-9.4

VIEW FROM
BACK



VIEW FROM SIDE



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE $1\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE $1\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

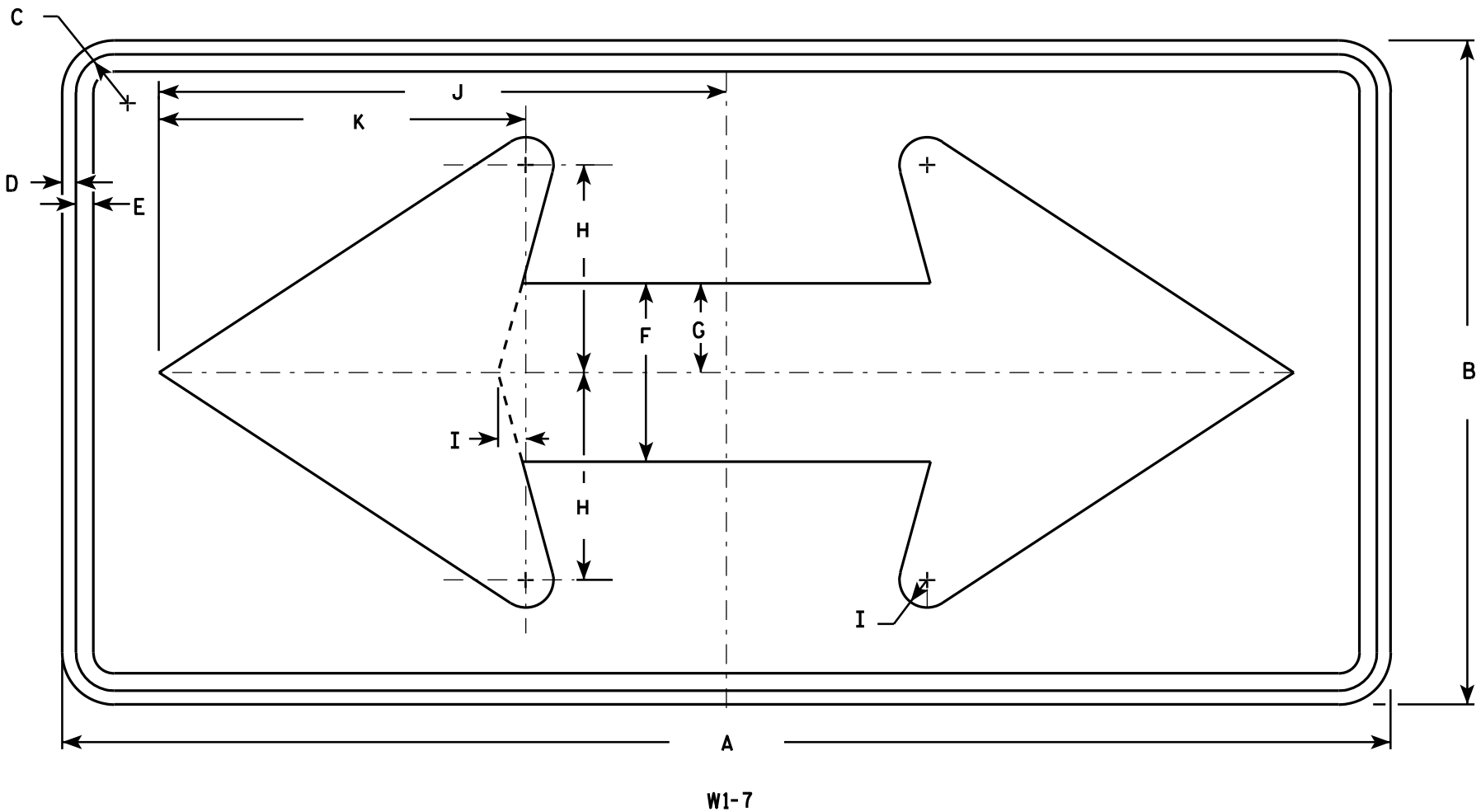
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-10.2

PROJECT NO:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|-------|-------|-------|-------|--------|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 5 | 2 1/2 | 5 3/4 | 3/4 | 15 5/8 | 10 1/8 | | | | | | | | | | | | | | | | 4.5 |
| 2S | 48 | 24 | 1 3/8 | 1/2 | 5/8 | 6 1/2 | 3 1/4 | 7 1/2 | 1 | 20 1/2 | 13 1/4 | | | | | | | | | | | | | | | | 8.0 |
| 2M | 48 | 24 | 1 3/8 | 1/2 | 5/8 | 6 1/2 | 3 1/4 | 7 1/2 | 1 | 20 1/2 | 13 1/4 | | | | | | | | | | | | | | | | 8.0 |
| 3 | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 8 | 4 | 9 1/4 | 1 1/4 | 25 3/8 | 16 1/4 | | | | | | | | | | | | | | | | 12.5 |
| 4 | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 8 | 4 | 9 1/4 | 1 1/4 | 25 3/8 | 16 1/4 | | | | | | | | | | | | | | | | 12.5 |
| 5 | 96 | 48 | 2 1/4 | 3/4 | 1 | 13 | 6 1/2 | 15 | 2 | 41 | 26 1/2 | | | | | | | | | | | | | | | | 32.0 |

STANDARD SIGN

W1 - 7

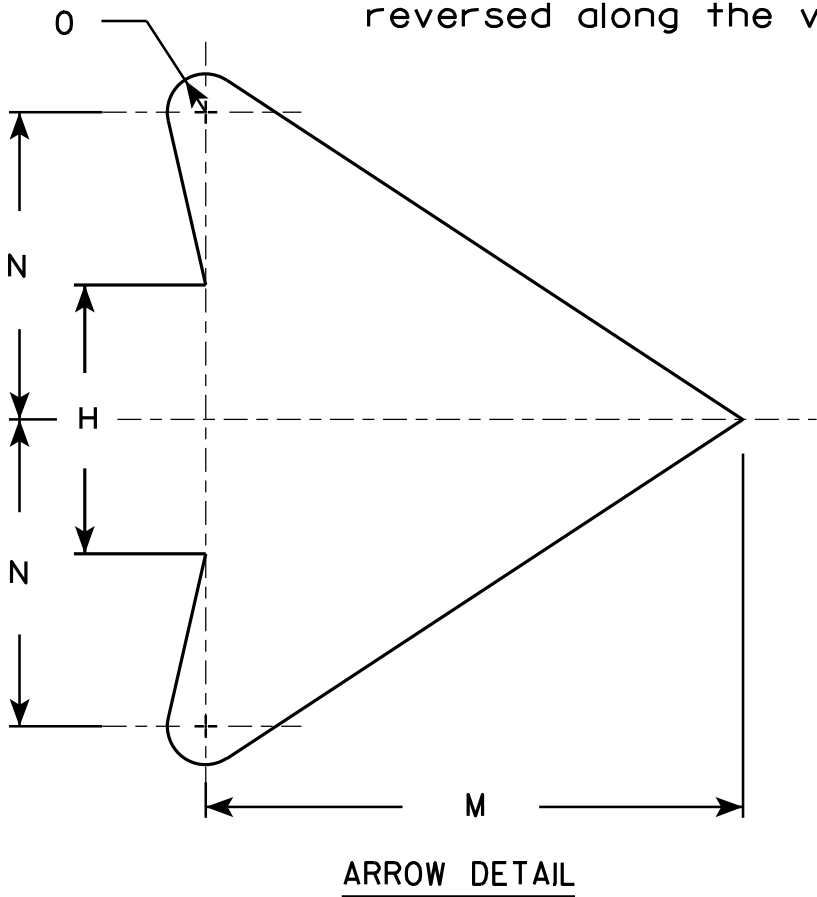
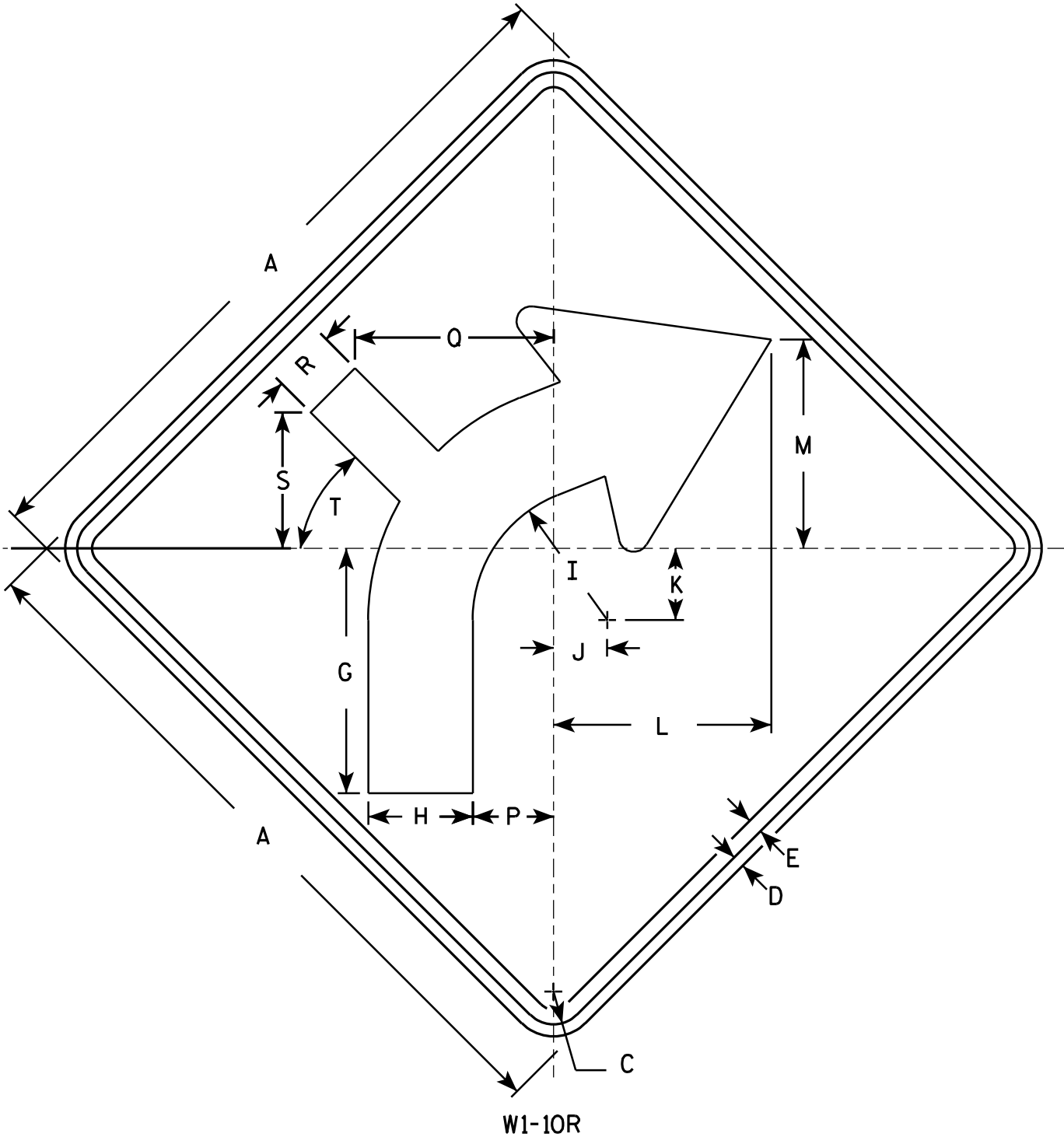
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-7.7

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W1-10L is the same as W1-10R except the arrow is reversed along the vertical centerline.



| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|--------|-------|-------|-------|-------|--------|--------|---|-----|-------|--------|-------|-------|-----|---|---|---|---|---|---|--------------|
| 1 | 24 | | 1 1/8 | 3/8 | 1/2 | | 8 1/4 | 3 1/2 | 4 1/2 | 1 3/4 | 2 3/8 | 7 1/4 | 7 | 4 | 1/2 | 2 5/8 | 6 5/8 | 2 1/8 | 4 1/2 | 45° | | | | | | | 4.0 |
| 2S | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 1/4 | 4 3/8 | 5 5/8 | 2 1/4 | 3 | 9 1/8 | 8 3/4 | 5 | 5/8 | 3 3/8 | 8 3/8 | 2 5/8 | 5 3/4 | 45° | | | | | | | 6.25 |
| 2M | 36 | | 1 5/8 | 5/8 | 3/4 | | 12 3/8 | 5 1/4 | 6 3/4 | 2 5/8 | 3 1/2 | 10 7/8 | 10 1/2 | 6 | 3/4 | 4 | 10 | 3 1/4 | 6 7/8 | 45° | | | | | | | 9.0 |
| 3 | 36 | | 1 5/8 | 5/8 | 3/4 | | 12 3/8 | 5 1/4 | 6 3/4 | 2 5/8 | 3 1/2 | 10 7/8 | 10 1/2 | 6 | 3/4 | 4 | 10 | 3 1/4 | 6 7/8 | 45° | | | | | | | 9.0 |
| 4 | 36 | | 1 5/8 | 5/8 | 3/4 | | 12 3/8 | 5 1/4 | 6 3/4 | 2 5/8 | 3 1/2 | 10 7/8 | 10 1/2 | 6 | 3/4 | 4 | 10 | 3 1/4 | 6 7/8 | 45° | | | | | | | 9.0 |
| 5 | 48 | | 2 1/4 | 3/4 | 1 | | 16 1/2 | 7 | 9 | 3 1/2 | 4 5/8 | 14 1/2 | 14 | 8 | 1 | 5 3/8 | 13 1/4 | 4 1/4 | 9 1/8 | 45° | | | | | | | 16.0 |

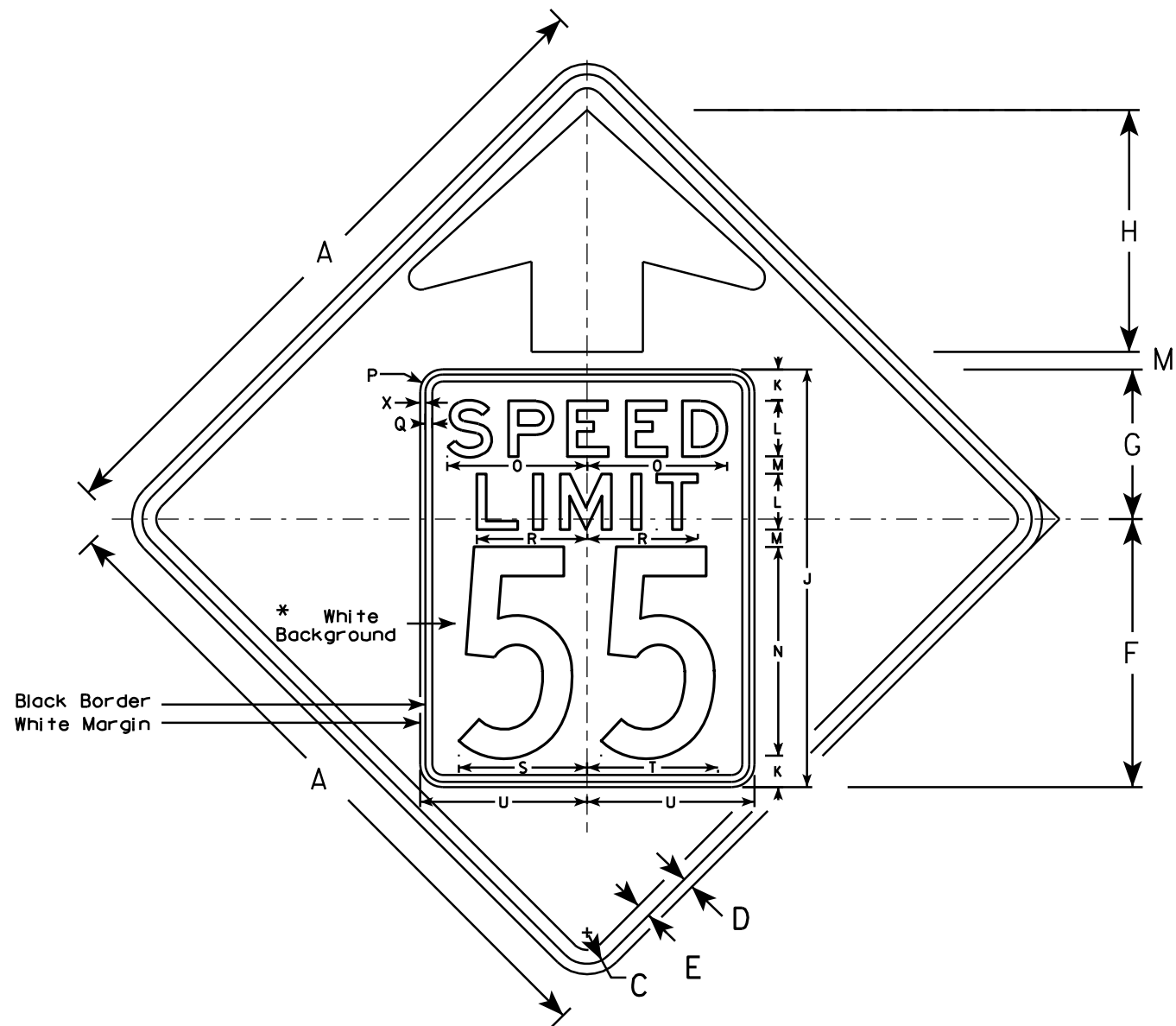
STANDARD SIGN
W1-10

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-10.3

PROJECT NO: HWY: COUNTY: SHEET NO: E

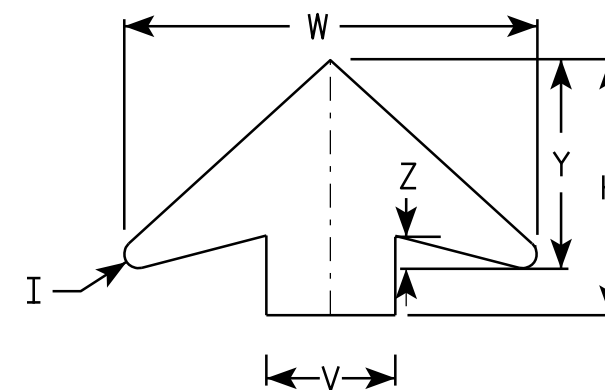


W3-5

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color: *
Background - YELLOW*
Message - BLACK
3. Message Series - C for numbers Series E for wording
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background



ARROW DETAIL

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|--------|--------|--------|-----|----|-------|---|-------|----|-------|-------|-----|-------|-------|-------|----|---|--------|-----|-------|-------|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 36 | | 1 5⁄8 | 5⁄8 | 3⁄4 | 14 1⁄2 | 9 1⁄2 | 11 1⁄2 | 5⁄8 | 24 | 2 | 3 | 1 | 12 | 7 1⁄8 | 1 1⁄2 | 3⁄8 | 5 3⁄4 | 7 1⁄4 | 7 1⁄8 | 9 | 6 | 19 1⁄4 | 3⁄8 | 9 3⁄4 | 1 5⁄8 | 9.0 |
| 2M | 36 | | 1 5⁄8 | 5⁄8 | 3⁄4 | 14 1⁄2 | 9 1⁄2 | 11 1⁄2 | 5⁄8 | 24 | 2 | 3 | 1 | 12 | 7 1⁄8 | 1 1⁄2 | 3⁄8 | 5 3⁄4 | 7 1⁄4 | 7 1⁄8 | 9 | 6 | 19 1⁄4 | 3⁄8 | 9 3⁄4 | 1 5⁄8 | 9.0 |
| 3 | 36 | | 1 5⁄8 | 5⁄8 | 3⁄4 | 14 1⁄2 | 9 1⁄2 | 11 1⁄2 | 5⁄8 | 24 | 2 | 3 | 1 | 12 | 7 1⁄8 | 1 1⁄2 | 3⁄8 | 5 3⁄4 | 7 1⁄4 | 7 1⁄8 | 9 | 6 | 19 1⁄4 | 3⁄8 | 9 3⁄4 | 1 5⁄8 | 9.0 |
| 4 | 48 | | 2 1⁄4 | 3⁄4 | 1 | 19 1⁄4 | 10 3⁄4 | 17 3⁄8 | 7⁄8 | 30 | 2 1⁄4 | 4 | 1 1⁄4 | 15 | 10 | 1 5⁄8 | 1⁄2 | 8 | 9 1⁄4 | 9 3⁄8 | 12 | 8 | 25 5⁄8 | 3⁄8 | 13 | 2 | 16.0 |
| 5 | 48 | | 2 1⁄4 | 3⁄4 | 1 | 19 1⁄4 | 10 3⁄4 | 17 3⁄8 | 7⁄8 | 30 | 2 1⁄4 | 4 | 1 1⁄4 | 15 | 10 | 1 5⁄8 | 1⁄2 | 8 | 9 1⁄4 | 9 3⁄8 | 12 | 8 | 25 5⁄8 | 3⁄8 | 13 | 2 | 16.0 |

STANDARD SIGN

W3-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

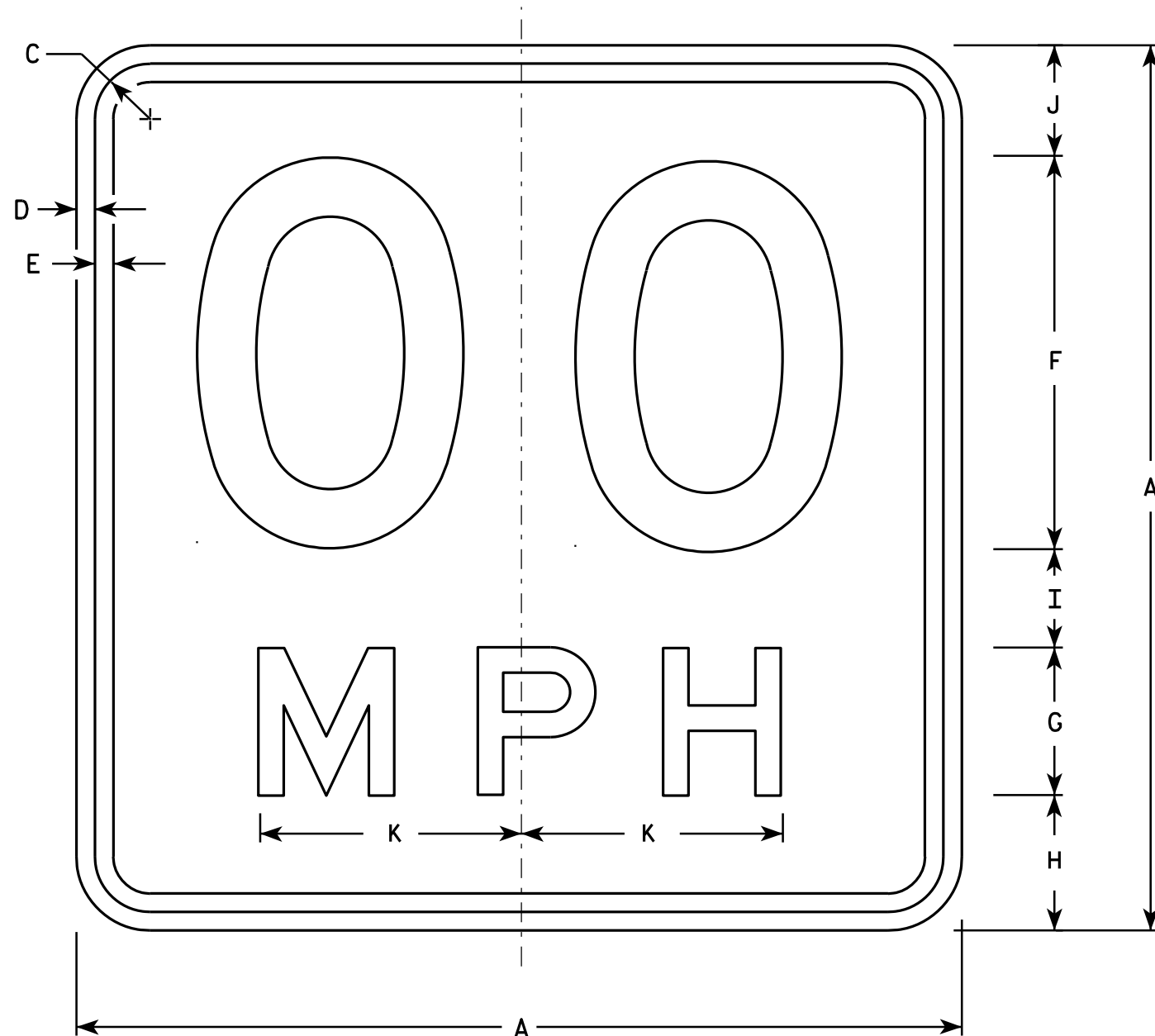
DATE 5/29/12

PLATE NO. W3-5.5

PROJECT NO:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

W13-1

- * For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | O | R | S | T | U | v | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|----|---|-------|-------|-------|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 18 | | 1 1/8 | 3/8 | 3/8 | 8 | 3 | 2 3/4 | 2 | 2 1/4 | 5 3/8 | | | | | | | | | | | | | | | | 2.25 |
| * 2S | 18 | | 1 1/8 | 3/8 | 3/8 | 8 | 3 | 2 3/4 | 2 | 2 1/4 | 5 3/8 | | | | | | | | | | | | | | | | 2.25 |
| * 2M | 18 | | 1 1/8 | 3/8 | 3/8 | 8 | 3 | 2 3/4 | 2 | 2 1/4 | 5 3/8 | | | | | | | | | | | | | | | | 2.25 |
| 3 | 24 | | 1 1/8 | 3/8 | 1/2 | 10 | 4 | 4 | 2 3/4 | 3 1/4 | 6 5/8 | | | | | | | | | | | | | | | | 4.00 |
| 4 | 36 | | 1 5/8 | 5/8 | 3/4 | 16 | 6 | 5 1/2 | 4 | 4 1/2 | 10 5/8 | | | | | | | | | | | | | | | | 9.00 |
| 5 | 36 | | 1 5/8 | 5/8 | 3/4 | 16 | 6 | 5 1/2 | 4 | 4 1/2 | 10 5/8 | | | | | | | | | | | | | | | | 9.00 |

STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

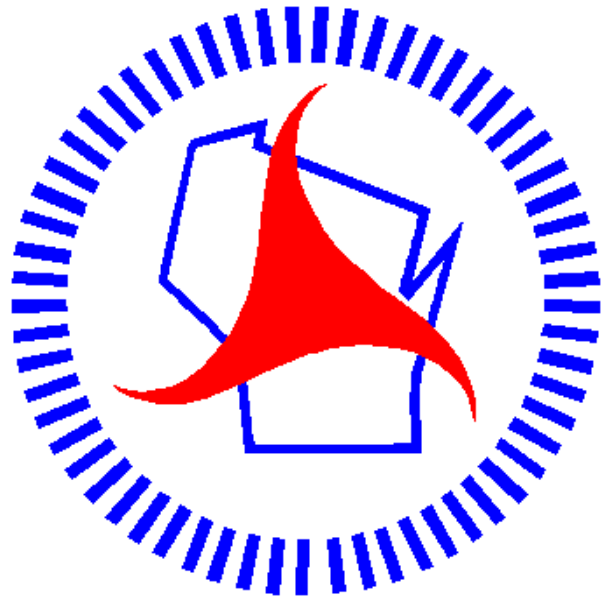
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>