

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7040-00-70	WISC 2020282	1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

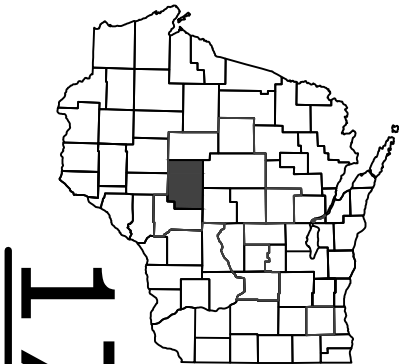
GREENWOOD - SPENCER

BRANCH ROCK CREEK CULVERT C-10-0017

STH 98

CLARK

STATE PROJECT NUMBER
7040-00-70



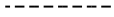



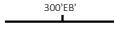
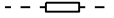




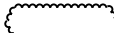


DESIGN DESIGNATION

A.A.D.T.	2017	=	2800
A.A.D.T.	2041	=	3300
D.H.V.		=	386
D.D.		=	60/40
T.		=	20.2
DESIGN SPEED		=	60
ESALS		=	1,200,000

CONVENTIONAL SYMBOLS

PLAN

CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

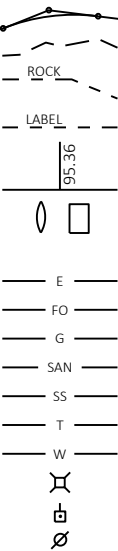
GRADE LINE
ORIGINAL GROUND
MARSH OR ROCK PROFILE
(To be noted as such)
SPECIAL DITCH

GRADE ELEVATION

CULVERT (Profile View)

UTILITIES

ELECTRIC
FIBER OPTIC
GAS
SANITARY SEWER
STORM SEWER
TELEPHONE
WATER
UTILITY PEDESTAL
POWER POLE
TELEPHONE POLE



T-27-N
↑
↓
T-26-N

END PROJECT
STA 498+61

EXISTING STRUCTURE C-10-0130
PROPOSED STRUCTURE C-10-0017


LAYOUT
 SCALE 0 1 MI
 TOTAL NET LENGTH OF CENTERLINE = 0.030 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), CLARK COUNTY NAD83 (YEAR, IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (YEAR, GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

<div style="display: flex; justify-content: space-between;"> <div style="width: 40%;"> <p style="margin: 0;">STATE OF WISCONSIN</p> <p style="margin: 0;">DEPARTMENT OF TRANSPORTATION</p> </div> <div style="width: 60%;"></div> </div>	
PREPARED BY	
Surveyor	<div style="border-bottom: 1px solid black; margin: 0 auto; width: 80%;">WISDOT</div>
Designer	<div style="border-bottom: 1px solid black; margin: 0 auto; width: 80%;">NATHAN ULLNESS</div>
Project Manager	<div style="border-bottom: 1px solid black; margin: 0 auto; width: 80%;">BRETT HOLLISTER</div>
Regional Examiner	<div style="border-bottom: 1px solid black; margin: 0 auto; width: 80%;">JENNIFER OLDENBURG</div>
Regional Supervisor	<div style="border-bottom: 1px solid black; margin: 0 auto; width: 80%;">JAMES KOENIG</div>

APPROVED FOR THE DEPARTMENT

DATE: 3/24/2020



(Signature)

E

LIST OF STANDARD ABBREVIATIONS

ABUT.	ABUTMENT
AGG.	AGGREGATE
AH.	AHEAD
APPROX.	APPROXIMATE
A.E.W.	APRON ENDWALL
ASPH.	ASPHALTIC
A.D.T.	AVERAGE DAILY TRAFFIC
AZ.	AZIMUTH
BK.	BACK
BEG.	BEGIN
B.M.	BENCH MARK
C/L	CENTER LINE
CONC.	CONCRETE
CONST.	CONSTRUCTION
CO.	COUNTY
C.T.H.	COUNTY TRUNK HIGHWAY
X-SEC.	CROSS SECTION
CR.	CRUSHED
CFS	CUBIC FEET/SECOND
C.Y., CU. YD.	CUBIC YARD
CULV.	CULVERT
C.P.	CULVERT PIPE
D.O.T.	DEPARTMENT OF TRANSPORTATION
D.H.V.	DESIGN HOUR VOLUME
DIA.	DIAMETER
D.	DIRECTIONAL DISTRIBUTION
DISCH. OR DIS.	DISCHARGE
EA.	EACH
ELECT.	ELECTRIC
EL. OR ELEV.	ELEVATION
EMB.	EMBANKMENT
E.B.S.	EXCAVATION BELOW SUBGRADE
EXIST.	EXISTING
FERT.	FERTILIZE
F.E.	FIELD ENTRANCE
FIN.	FINISHED
FT.	FOOT
F.L.	FLOW LINE
GA.	GAUGE
HORIZ.	HORIZONTAL
CWT.	HUNDREDWEIGHT
INL.	INLET
LT.	LEFT
L.H.F.	LEFT-HAND FORWARD
LIN.	LINEAR
LIN. FT.	LINEAR FOOT
L.S.	LUMP SUM
MAX.	MAXIMUM
MI.	MILE
MISC.	MISCELLANEOUS
N.E.	NORTH EAST
N.W.	NORTH WEST
PAV'T	PAVEMENT
P.C.	POINT OF CURVATURE
P.I.	POINT OF INTERSECTION
P.T.	POINT OF TANGENCY
P.O.T.	POINT ON TANGENT
LB.	POUND
P.E.	PRIVATE ENTRANCE
PROJ.	PROJECT
R.	RANGE
REQ'D	REQUIRED
RT.	RIGHT
R.H.F.	RIGHT-HAND FORWARD
R/W	RIGHT OF WAY
RD.	ROAD
SHR.	SHRINKAGE
SL.	SLOPE
STD.	STANDARD
S.D.D.	STANDARD DETAIL DRAWINGS
S.T.H.	STATE TRUNK HIGHWAY
STA.	STATION
S.P.P.A.	STRUCTURAL PLATE PIPE ARCH
STRUCT.	STRUCTURE
SURF.	SURFACE
TEL.	TELEPHONE
TN.	TOWN
T.	TRUCKS (PERCENT OF)
UNCL.	UNCLASSIFIED
U.G.	UNDERGROUND
V.	VELOCITY OR DESIGN SPEED
V.C.	VERTICAL CURVE

GENERAL NOTES

WHEN THE QUANTITY OF THE ITEMS OF SUBBASE OR SURFACE COURSE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER.

DISTURBED AREAS WITHIN THE RIGHT OF WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, ARE TO BE SALVAGED TOPSOILED, SEEDED, AND MULCHED AS DIRECTED BY THE ENGINEER.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE QUANTITY OF THE ITEMS FOR EROSION PROTECTION INCLUDES UNDISTRIBUTED AMOUNTS. THE DISTRUBUTION AND LOCATION OF THESE MATERIALS ARE TO BE DETERMINED BY THE ENGINEER.

THE LOCATION OF EXISTING UTILITY LOCATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE SOME OTHER UTILITY INSTALLATIONS WITHIN THE AREA THAT ARE NOT SHOWN. IT IS THE CONCRACTOR'S RESPONSIBILITY TO CALL DIGGERS HOTLINE PRIOR TO BEGINNING WORK OPERATIONS. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

ALL TEMPORARY STOCK PILES MUST BE IN AN UPLAND LOCATION AND PROTECTED WITH EROSION CONTROL MEASURES. DO NOT STOCKPILE MATERIALS IN WETLANDS, WATERWAYS, OR FLOODPLAIN'S.

DIGGERS



HOTLINE

Dial  or (800)242-8511

www.DiggersHotline.com

DEPARTMENT OF NATURAL RESOURCES

DNR WEST CENTRAL REGION
1300 W CLAIRMONT AVENUE
EAU CLAIRE, WI 54701
TELEPHONE: 715-934-9014
ATTN: LEAH NICOL
LEAH.NICOL@WISCONSIN.GOV

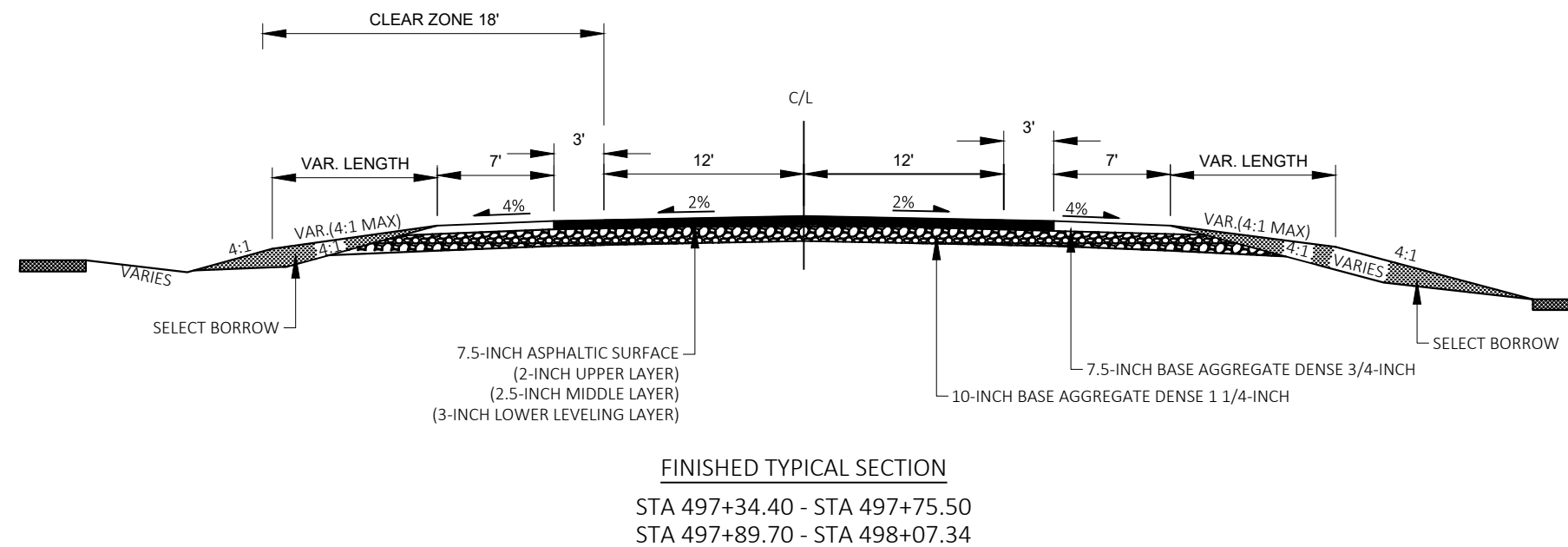
UTILITIES

ASTREA F/K/A PACKERLAND BROADBAND
ANDY HEIGL
105 KENT ST
PO BOX 190
IRON MOUNTAIN, MI 49801-0190
906-774-6621 (OFFICE)
906-221-7536 (MOBILE)
andy.heigl@astreaconnect.com

CHARTER SPECTRUM
JESSE GRUNY
SUITE 103
508 N CENTRAL AVE
MARSHFIELD, WI 54449
715-370-4171 (OFFICE)
715-651-5605 (MOBILE)
jesse.gruny@charter.com

FRONTIER COMMUNICATIONS
RUSS RYAN
521 N 4TH ST
WAUSAU, WI 54403
920-583-3275 (OFFICE)
920-737-9662 (MOBILE)
russell.w.ryan@ftr.com

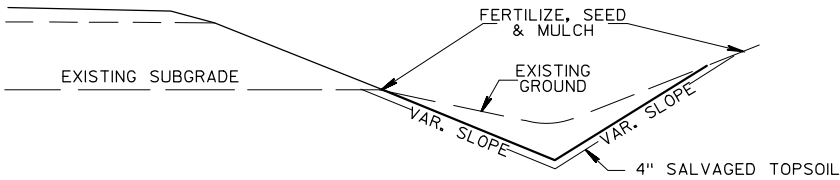
DESIGN CONTACT
WISDOT
NATHAN ULNESS
715-563-4631 (MOBILE)
Nathan.Ulness@dot.wi.gov



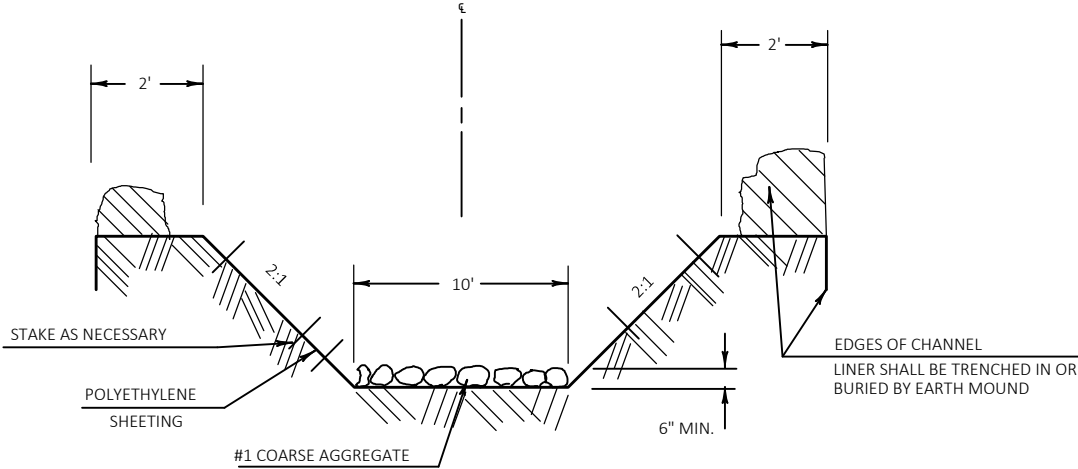
RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP- TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE- TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = ----- ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = -----ACRES

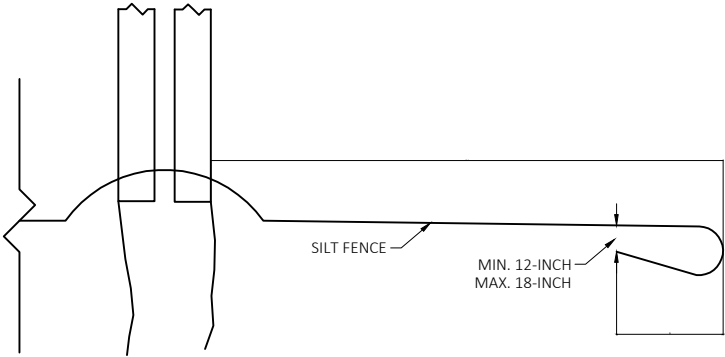


DITCH SHAPING DETAIL

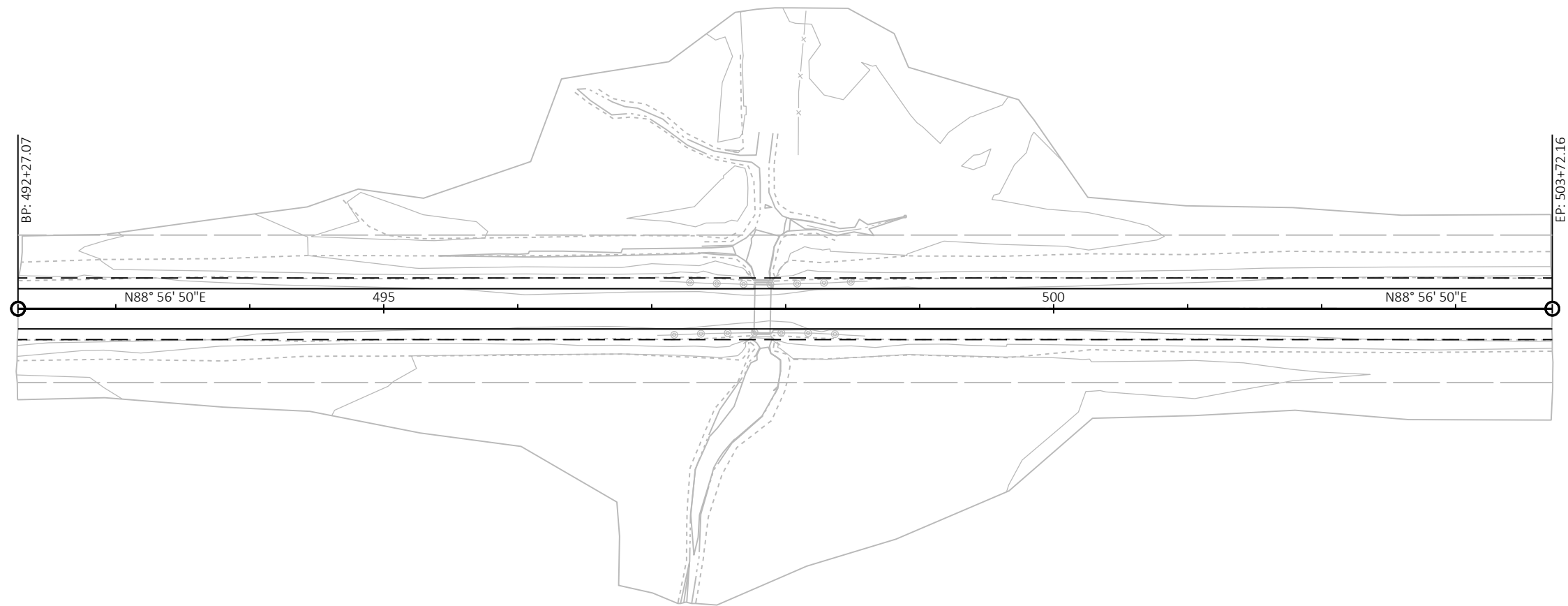


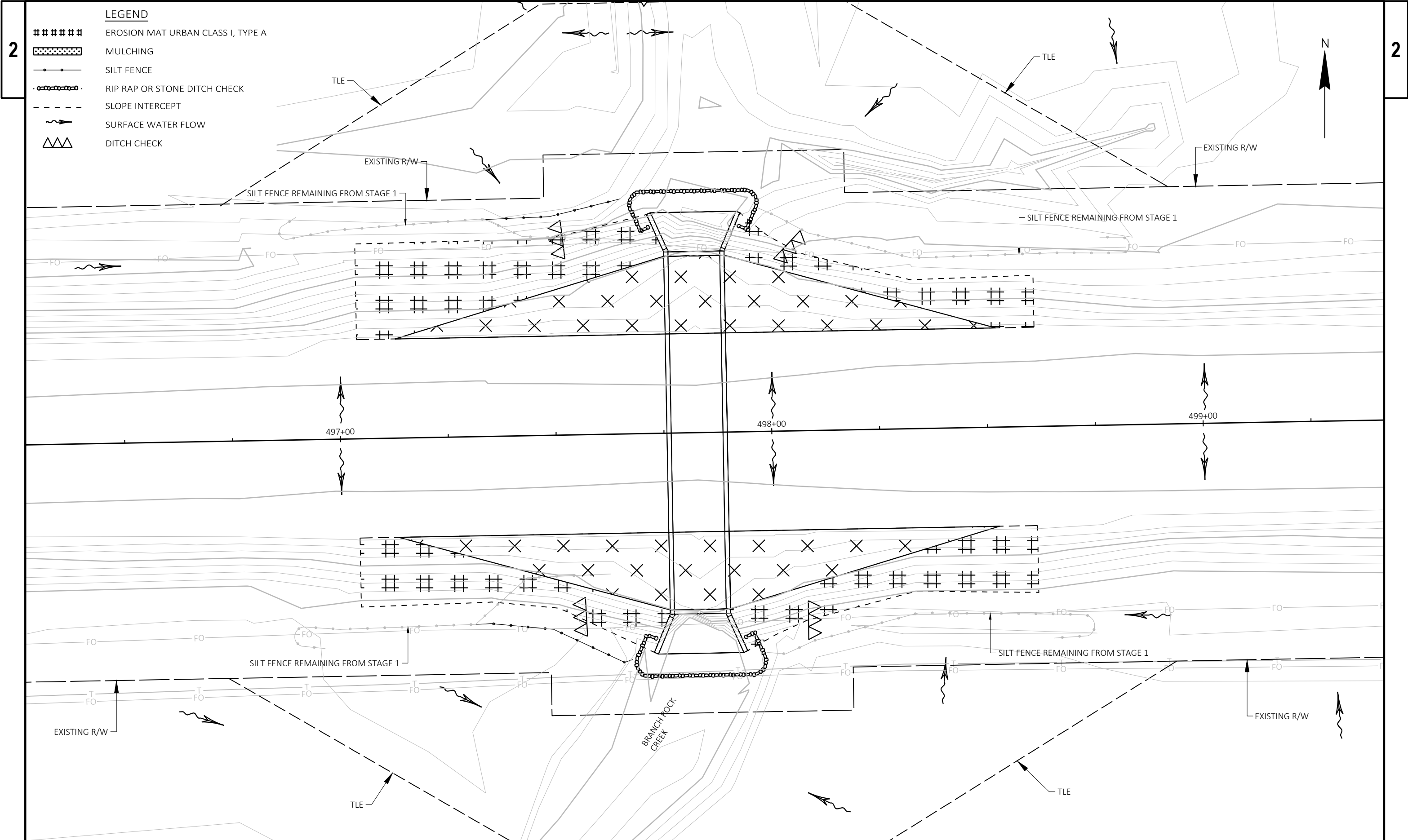
TYPICAL SECTION OF TEMPORARY CHANNEL CHANGE

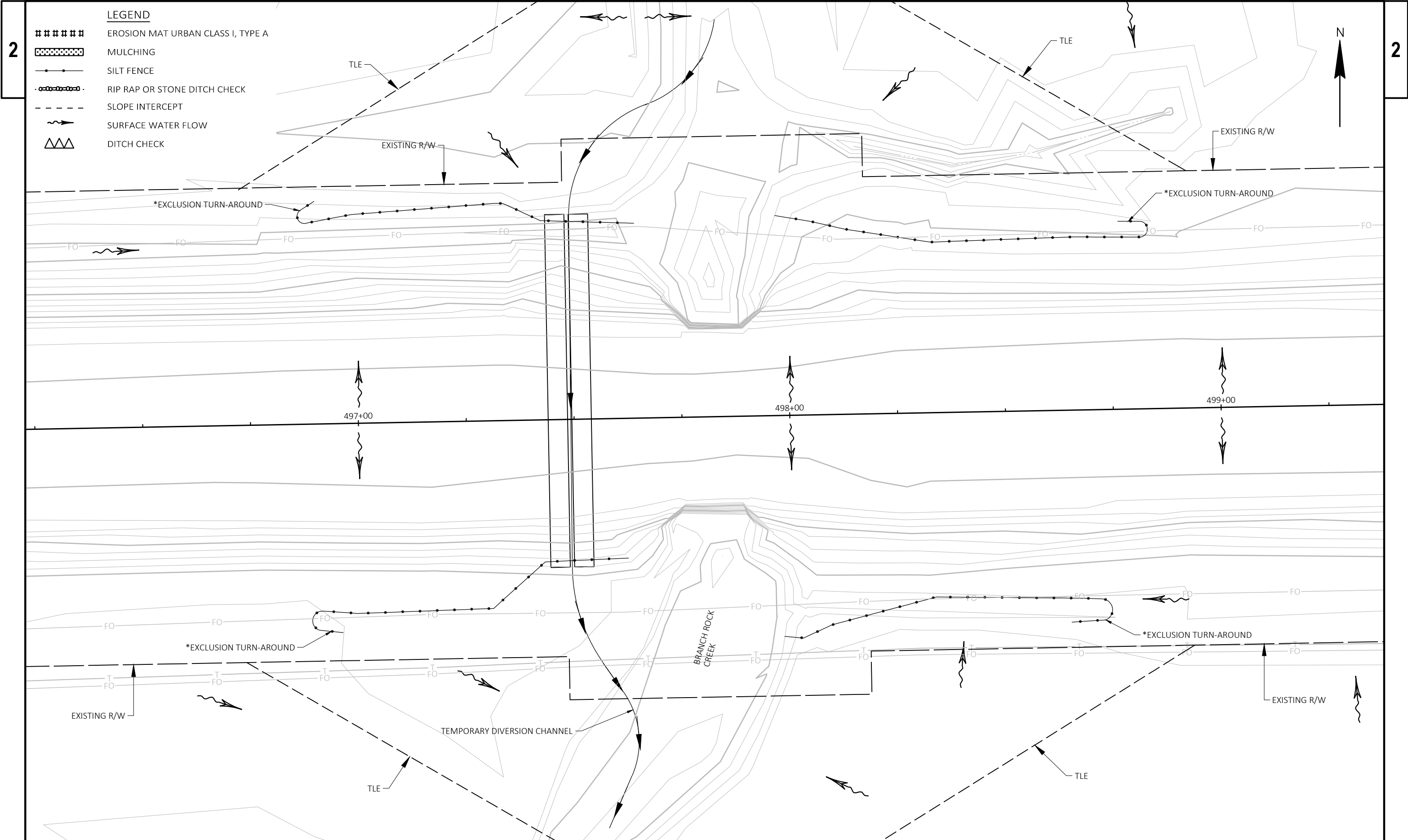
NOTE:
ALL ITEMS USED TO DIVERT CHANNEL ARE INCIDENTAL TO THE BID ITEM 'TEMPORARY WATER DIVERSION, BRANCH ROCK CREEK'



EXCLUSION FENCING TURN-AROUND

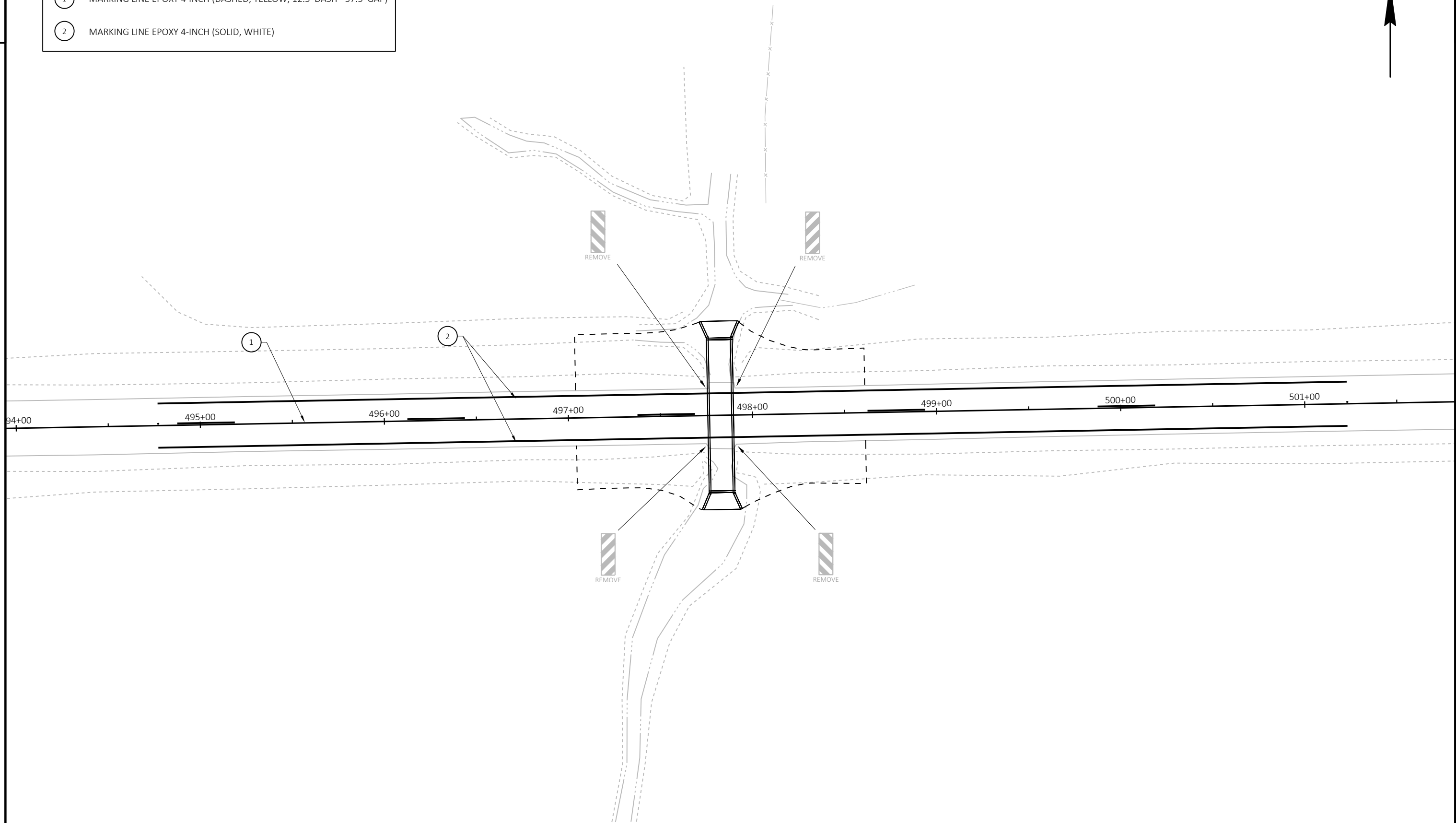






LEGEND

- 1 MARKING LINE EPOXY 4-INCH (DASHED, YELLOW, 12.5' DASH - 37.5' GAP)
- 2 MARKING LINE EPOXY 4-INCH (SOLID, WHITE)



LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON TEMPORARY SUPPORT
- CONCRETE BARRIER TEMPORARY PRECAST
- WORK AREA
- DIRECTION OF TRAFFIC
- TEMPORARY TRAFFIC SIGNAL WITH BACKPLATE AND 12-INCH LENSES MOUNTED ON TRAILER

TRAFFIC CONTROL GENERAL NOTES:

1.) SEE STANDARD DETAIL DRAWING 15 D 33 "TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS" AND STANDARD DETAIL DRAWING 9 G 02-C "BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION" FOR TRAFFIC CONTROL DETAILS. NOT SHOWN ON THIS DETAIL DRAWING.

SEE STANDARD DETAIL DRAWING 14 B 7-15 "CONCRETE BARRIER TEMPORARY PRECAST" FOR BARRIER END TREATMENT DETAILS (TYP.)

STAGE 1 AND STAGE 2 DESIGNATIONS FOR CONSTRUCTION ARE FOR INFORMATIONAL PURPOSES ONLY AND ARE NOT INDICATIVE OF THE ORDER IN WHICH WORK WILL BE PERFORMED.

THE CONTRACTOR SHALL REVIEW SIGNALS AFTER PROGRAMMING TO ASSURE THERE ARE NO CONFLICTING MOVEMENTS. ONCE THE TEMPORARY SIGNAL HAS BEEN ACTIVATED, THE CONTRACTOR SHALL REVIEW THE TIMINGS DURING THE PEAK HOURS AND NOTIFY THE DEPARTMENT OF ANY RECOMMENDED TIMING CHANGES TO OPTIMIZE THE TRAFFIC SIGNAL, IF NEEDED.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

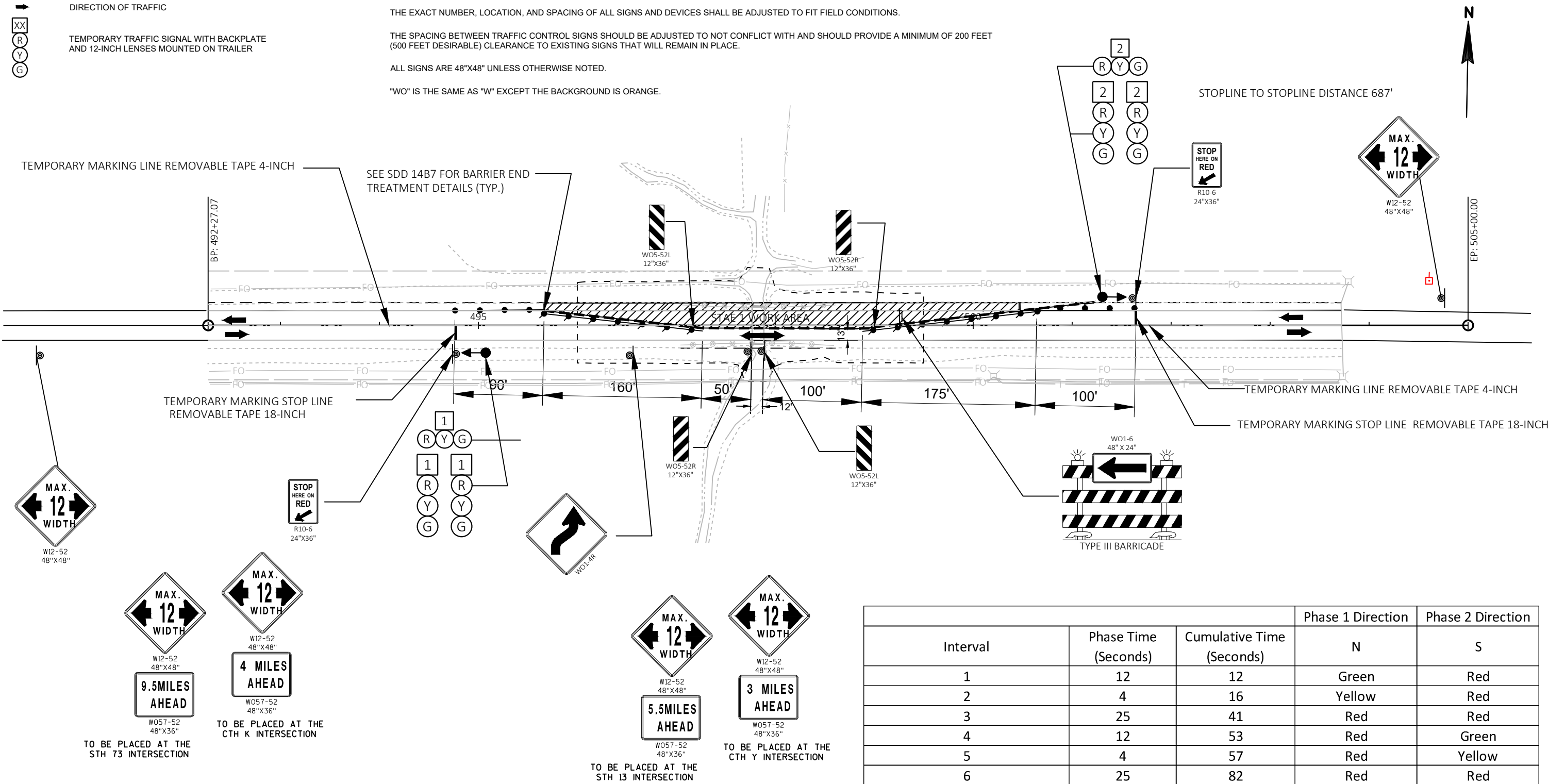
ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

CONTINUED:

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE, AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.



			Phase 1 Direction	Phase 2 Direction
			N	S
Interval	Phase Time (Seconds)	Cumulative Time (Seconds)		
1	12	12	Green	Red
2	4	16	Yellow	Red
3	25	41	Red	Red
4	12	53	Red	Green
5	4	57	Red	Yellow
6	25	82	Red	Red

LEGEND

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- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
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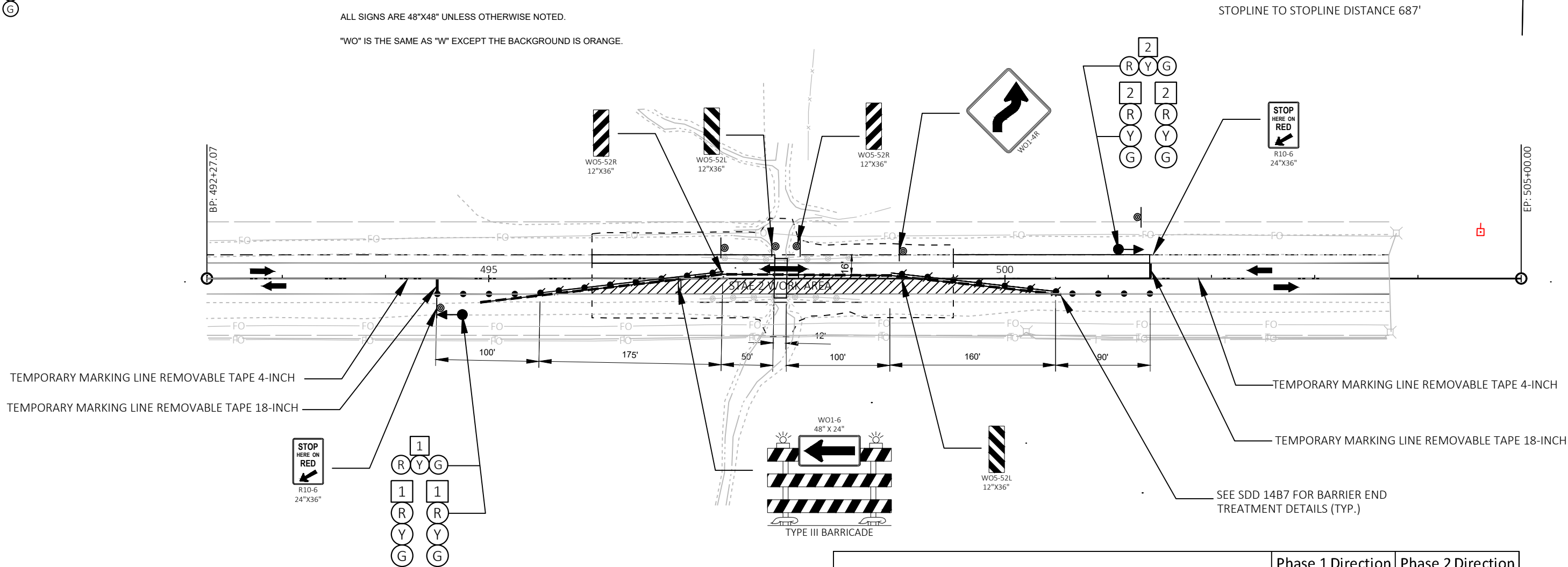
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3	25	41	Red	Red
4	12	53	Red	Green
5	4	57	Red	Yellow
6	25	82	Red	Red

Alignment: Alignment -STH 98
Description:
Layer: P_ALI_Alignment -STH 98

Tangent

N416364.372 E731551.218 Z

Station 492+27.07

Distance 1322.930'

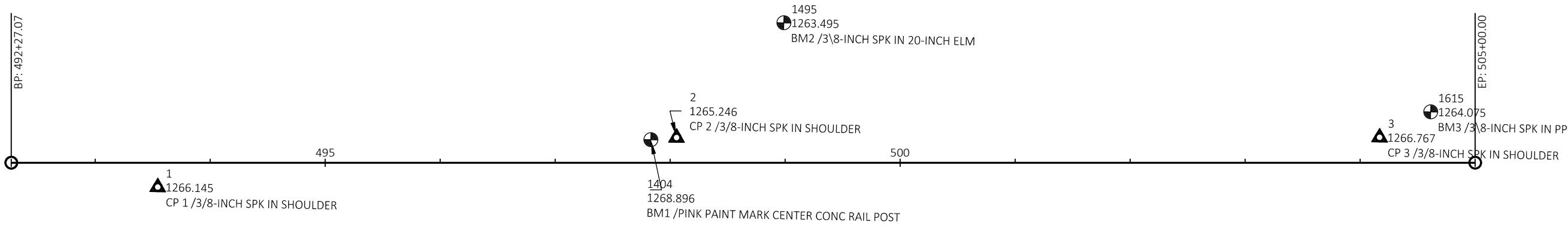
Bearing N 88° 56' 49.66 E"

End

N416388.681 E732873.925 Z

Station 505+50

Alignment Length = 1322.93'



CONTROL POINTS						
POINTS	STATION	OFFSET	COORDINATE X	COORDINATE Y	ELEVATION	DESCRIPTION
CP1	493+54.43	21.16'	731678.94	416345.55	1266.14	3/8-INCH SPK 6' SOUTH OFSOUTH EOB STH 98 IN SHOULDER
CP2	498+05.57	-21.18'	732129.79	416396.79	1265.25	3/8-INCH SPK IN NORTH SHOULDER OF STH 98 AT NE CORNER BRIDGE
CP3	504+16.94	-22.00'	732740.48	416408.23	1266.76	3/8-INCH SPK IN GRAVEL SHOULDER NORTH SIDE STH 98 6' NORTH EOB

BENCHMARKS			
POINTS	STATION	OFFSET	ELEVATION
BM1	497+83.18	-20.05'	1268.89
BM2	498+98.87	-121.42'	1263.49
BM3	504+61.36	-44.18'	1264.07

Estimate Of Quantities

7040-00-70					
Line	Item	Item Description	Unit	Total	Qty
0002	203.0200	Removing Old Structure (station) 01. 497+82.7	LS	1.000	1.000
0004	204.0100	Removing Pavement	SY	146.000	146.000
0006	204.0165	Removing Guardrail	LF	312.000	312.000
0008	206.2000	Excavation for Structures Culverts (structure) 01. C-10-17	LS	1.000	1.000
0010	208.1100	Select Borrow	CY	243.000	243.000
0012	210.2500	Backfill Structure Type B	TON	790.000	790.000
0014	213.0100	Finishing Roadway (project) 01. 7040-00-70	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	58.000	58.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	238.000	238.000
0020	311.0115	Breaker Run	CY	80.000	80.000
0022	455.0605	Tack Coat	GAL	52.000	52.000
0024	465.0105	Asphaltic Surface	TON	106.000	106.000
0026	465.0125	Asphaltic Surface Temporary	TON	12.000	12.000
0028	504.0100	Concrete Masonry Culverts	CY	148.000	148.000
0030	505.0400	Bar Steel Reinforcement HS Structures	LB	25,560.000	25,560.000
0032	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	640.000	640.000
0034	511.1200	Temporary Shoring (structure) 01. C-10-0017	SF	70.000	70.000
0036	516.0500	Rubberized Membrane Waterproofing	SY	24.000	24.000
0038	520.2054	Culvert Pipe Temporary 54-Inch	LF	164.000	164.000
0040	603.8000	Concrete Barrier Temporary Precast Delivered	LF	552.000	552.000
0042	603.8125	Concrete Barrier Temporary Precast Installed	LF	1,104.000	1,104.000
0044	606.0300	Riprap Heavy	CY	39.000	39.000
0046	614.0360	Steel Plate Beam Guard Temporary	LF	60.000	60.000
0048	618.0100	Maintenance And Repair of Haul Roads (project) 01. 7040-00-70	EACH	1.000	1.000
0050	619.1000	Mobilization	EACH	1.000	1.000
0052	624.0100	Water	MGAL	4.600	4.600
0054	625.0500	Salvaged Topsoil	SY	308.000	308.000
0056	627.0200	Mulching	SY	308.000	308.000
0058	628.1504	Silt Fence	LF	446.000	446.000
0060	628.1520	Silt Fence Maintenance	LF	446.000	446.000
0062	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0064	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0066	628.2006	Erosion Mat Urban Class I Type A	SY	348.000	348.000
0068	628.7504	Temporary Ditch Checks	LF	40.000	40.000
0070	629.0210	Fertilizer Type B	CWT	0.200	0.200
0072	630.0110	Seeding Mixture No. 10	LB	5.000	5.000
0074	630.0500	Seed Water	MGAL	7.000	7.000
0076	638.2602	Removing Signs Type II	EACH	4.000	4.000

Estimate Of Quantities

7040-00-70					
Line	Item	Item Description	Unit	Total	Qty
0078	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0080	642.5001	Field Office Type B	EACH	1.000	1.000
0082	643.0300	Traffic Control Drums	DAY	4,408.000	4,408.000
0084	643.0420	Traffic Control Barricades Type III	DAY	232.000	232.000
0086	643.0715	Traffic Control Warning Lights Type C	DAY	3,712.000	3,712.000
0088	643.0900	Traffic Control Signs	DAY	2,668.000	2,668.000
0090	643.5000	Traffic Control	EACH	1.000	1.000
0092	645.0105	Geotextile Type C	SY	253.000	253.000
0094	645.0120	Geotextile Type HR	SY	99.000	99.000
0096	646.1020	Marking Line Epoxy 4-Inch	LF	1,467.000	1,467.000
0098	646.9000	Marking Removal Line 4-Inch	LF	1,467.000	1,467.000
0100	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	1,000.000	1,000.000
0102	649.0850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	60.000	60.000
0104	650.4500	Construction Staking Subgrade	LF	73.000	73.000
0106	650.5000	Construction Staking Base	LF	73.000	73.000
0108	650.6500	Construction Staking Structure Layout (structure) 01. C-10-0017	LS	1.000	1.000
0110	650.9910	Construction Staking Supplemental Control (project) 01. 7040-00-70	LS	1.000	1.000
0112	650.9920	Construction Staking Slope Stakes	LF	175.000	175.000
0114	661.0100	Temporary Traffic Signals for Bridges (structure) 01. C-10-0017	LS	1.000	1.000
0116	690.0150	Sawing Asphalt	LF	30.000	30.000
0118	690.0250	Sawing Concrete	LF	114.000	114.000
0120	715.0502	Incentive Strength Concrete Structures	DOL	1,480.000	1,480.000
0122	SPV.0105	Special 01. Temporary Diversion Channel, Branch Rock Creek	LS	1.000	1.000

3

REMOVING PAVEMENT

204. 0100 REMOVING PAVEMENT					
STATION	TO	STATION	LOCATION	SY	REMARKS
497+34	-	498+07	STH 98	146	MAINLINE WITH EXISTING PCC
PROJECT TOTALS				146	

REMOVING GUARDRAIL

204. 0165 REMOVING GUARDRAIL					
STATION	TO	STATION	LOCATION	LF	REMARKS
497+06	-	498+61	STH 98	156	LT
497+04	-	498+59	STH 98	156	RT
PROJECT TOTAL				312	

BASE AGGREGATE DENSE 3/4-INCH

				305. 0110	624. 0100	
				BASE AGGREGATE	WATER	
				DENSE 3/4 INCH		
STATION	TO	STATION	LOCATION	TON	MGAL	REMARKS
497+34	-	498+07	EB SHOULDER	29	0.5	STAGE 1
497+34	-	498+07	WB SHOULDER	29	0.5	STAGE 2
PROJECT TOTALS				58	1.0	

BASE AGGREGATE DENSE 1 1/4-INCH

				305. 0120	624. 0100	
				BASE AGGREGATE	WATER	
				DENSE 1 1/4 INCH		
STATION	TO	STATION	LOCATION	TON	MGAL	REMARKS
497+34	-	498+07	EB MAINLINE	68	1.0	STAGE 1
497+34	-	498+07	EB SHOULDER	51	0.8	STAGE 1
497+34	-	498+07	WB MAINLINE	68	1.0	STAGE 2
497+34	-	498+07	WB SHOULDER	51	0.8	STAGE 2
PROJECT TOTALS				238	3.6	

ASPHALTIC SURFACE

465. 0105 ASPHALTIC SURFACE					
STATION	TO	STATION	LOCATION	TON	REMARKS
497+34	-	498+07	EB MAINLINE	53	STAGE 1
497+34	-	498+07	WB MAINLINE	53	STAGE 2
PROJECT TOTALS				106	

SELECT BORROW

208. 1100 SELECT BORROW					
STATION	TO	STATION	LOCATION	CY	REMARKS
497+04	-	498+61	STH 98	243	MAINLINE
TOTAL 0010				243	

FINISHING ROADWAY (7040-00-70)

213. 0100 FINISHING ROADWAY			REMARKS
LOCATION	EACH		
STH 98	1		PROJECT
PROJECT TOTALS		1	

TACK COAT

455. 0605 TACK COAT					
STATION	TO	STATION	LOCATION	GAL	REMARKS
497+34	-	498+07	EB MAINLINE	26	STAGE 1
497+34	-	498+07	WB MAINLINE	26	STAGE 2
PROJECT TOTALS				52	

ASPHALTIC SURFACE TEMPORARY

465. 0125 ASPHALTIC SURFACE TEMPORARY					
STATION	TO	STATION	LOCATION	TON	REMARKS
497+34	-	497+57	EB LANE	12	STAGE 1
PROJECT TOTALS				12	

3

3

CULVERT PIPE TEMPORARY 54-INCH

520. 2054 CULVERT PIPE TEMPORARY 54-INCH		
LOCATION	LF	REMARKS
STH 98	164	STAGE 1
PROJECT TOTALS	164	

CONCRETE BARRIER TEMPORARY PRECAST

STATION TO STATION		LOCATION	603. 8000 DELIVERED LF	603. 8125 INSTALLED LF	REMARKS
495+66	-	500+49	WB	552	STAGE 1
495+66	-	500+49	EB	552	STAGE 2
PROJECT TOTALS			552	1104	

SILT FENCE

STATION TO STATION		LOCATION	628. 1504 SILT FENCE LF	628. 1520 MAINTENANCE LF	REMARKS
496+87	-	498+84	NORTH SLOPE	185	STAGE 1
496+89	-	498+75	SOUTH SLOPE	191	STAGE 1
497+34	-	497+66	NORTH SLOPE	32	STAGE 1
497+31	-	497+67	SOUTH SLOPE	38	STAGE 2
PROJECT TOTALS			446	446	

LANDSCAPING

STATION TO STATION		LOCATION	625. 0500 SALVAGED TOPSOIL SY	627. 0200 MULCHING SY	628. 2006 EMAT URBAN CLASS I TYPE A SY	628. 7504 TEMPORARY DITCH CHECKS LF	629. 0210 FERTILIZER TYPE B CWT	630. 0110 SEEDING NO. 10 LB	630. 0500 SEED WATER MGAL
497+13	-	498+53	NORTH SLOPE	154		10	0. 1	2. 5	3. 5
497+13	-	498+53	SOUTH SLOPE	154		10	0. 1	2. 5	3. 5
497+04	-	498+61	NORTH SLOPE		183	10			
497+04	-	498+61	SOUTH SLOPE		165	10			
PROJECT TOTALS			308	308	348	7	0. 2	5	7. 0

FIELD OFFICE TYPE B

LOCATION		642. 5001 FIELD OFFICE TYPE B EACH
PROJECT	STH 85	1
PROJECT TOTALS		1

TRAFFIC CONTROL (7040-00-70)

LOCATION		643. 5000 TRAFFIC CONTROL EACH
STH 98		1
PROJECT TOTALS		1

MAINTENANCE AND REPAIR OF HAUL ROADS

LOCATION		618. 0100 EACH
PROJECT	STH 85	1
PROJECT TOTALS		1

STEEL PLATE BEAM GUARD TEMPORARY

				614. 0360	
				STEEL PLATE	
				BEAM GUARD TEMPORARY	
STATION TO	STATION	LOCATION		LF	REMARKS
497+30	-	497+60	EB SHOULDER	60	STAGE 1
PROJECT TOTALS				60	

3

TRAFFIC CONTROL

	643. 0300 DRUMS	643. 0420 BARRI CADES TYPE III	643. 0715 WARNI NG LI GHTS TYPE C	643. 0900 SI GNS	
LOCATI ON	DAY	DAY	DAY	DAY	REMARKS
STH 98	2204	116	1856	1624	STAGE 1
STH 98	2204	116	1856	1044	STAGE 2
PROJECT TOTALS	4408	232	3712	2668	

PAVEMENT MARKINGS

STATION TO	STATION	LOCATI ON	646. 9000 MARKI NG REMOVAL LI NE 4-I NCH LF	646. 1020 MARKI NG LI NE EPOXY 4-I NCH LF	REMARKS
494+77 -	501+23	STH 98	175	175	CENTERLI NE
494+77 -	501+23	WB EDGELI NE	646	646	STAGE 1
494+77 -	501+23	EB EDGELI NE	646	646	STAGE 2
PROJECT TOTALS			1467	1467	

TEMPORARY MARKING LINE

STATION TO	STATION	LOCATI ON	649. 0150 REMOVABLE TAPE 4-I NCH LF	649. 0850 REMOVABLE TAPE 18-I NCH LF	REMARKS
495+65 -	500+65	CENTERLI NE	500		STAGE 1
	494+75	CENTERLI NE		30	STAGE 1
495+50 -	500+50	CENTERLI NE	500		STAGE 2
	501+41	CENTERLI NE		30	STAGE 2
PROJECT TOTALS			1000	60	

CONSTRUCTION STAKI NG

STATION TO	STATION	LOCATI ON	650. 4500 SUBGRADE LF	650. 5000 BASE LF	650. 6500 STRUCTURE LAYOUT (C-10-0017) LS	650. 9910 SUPPLEMENTAL CONTROL (7040-00-70) LS	650. 9920 SLOPE STAKES LF	REMARKS
497+34 -	498+07	STH 98	73	73				CATEGORY 10
497+00 -	498+75	STH 98				1	175	CATEGORY 10
	497+83	STH 98			1			CATEGORY 20
PROJECT TOTALS			73	73	1	1	175	

SAWI NG

STATION TO	STATION	LOCATI ON	690. 0150 ASPHALT LF	690. 0250 CONCRETE LF	REMARKS
	497+34	STH 98	10	20	STAGE 1
	497+54	STH 98	10	20	STAGE 1
497+54 -	498+08	2FT RT OF CL	-	54	STAGE 1
	498+08	STH 98 WB	5	12	STAGE 1
	498+08	STH 98 EB	5	8	STAGE 2
PROJECT TOTALS			30	114	

REMOVI NG SI GNS

STATION TO	STATION	LOCATI ON	638. 2602 TYPE II EACH	638. 3000 SMALL SUPPORTS EACH	REMARKS
497+77 -	497+89	STH 98	2	2	LT
497+77 -	497+89	STH 98	2	2	RT
PROJECT TOTALS			4	4	

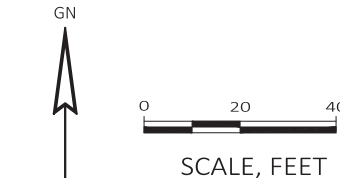
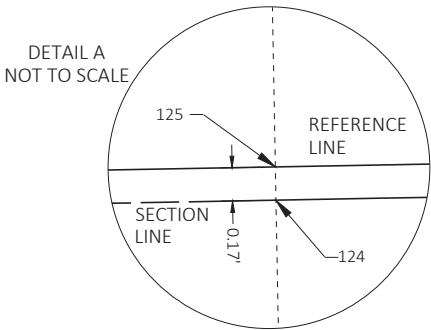
SPECIAL 01. TEMPORARY DIVERSION CHANNEL, BRANCH ROCK CREEK

LOCATI ON	SPV. 0105 TEMPORARY DI VERSI ON CHANNEL LS
STH 98	1
PROJECT TOTALS	1

TEMPORARY TRAFFIC SIGNALS FOR BRIDGES (C-10-0017)

LOCATI ON	661. 0100 LS
STH 98	1
PROJECT TOTALS	1

CONVENTIONAL SYMBOLS			
SECTION LINE	---	PARCEL NUMBER 25	UTILITY NUMBER 40
QUARTER LINE	---	SECTION CORNER	R/W MONUMENT
SIXTEENTH LINE	---	NOTATION FOR COMBUSTIBLE FLUIDS	NON-MONUMENTED R/W POINT
NEW REFERENCE LINE	---	NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES	FOUND IRON PIN
NEW R/W LINE	---	CAUTION	VALVE (GAS, WATER, ETC.)
EXISTING R/W LINE	---		SIGN
PROPERTY LINE	---		OFF-PREMISE SIGN
LOT, TIE, AND OTHER MINOR LINES	---		
SLOPE INTERCEPT	---		
CORPORATE LIMITS	---		
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---		
FEE ACQUISITION AREA (HATCHING VARIES BY OWNER)	---		
TEMP. LIMITED EASEMENT AREA	---		
EASEMENT AREA (HIGHWAY, PERMANENT LIMITED, OR RESTRICTED DEVELOPMENT)	---		
TRANSMISSION STRUCTURES	---		
BUILDING	---		
BUILDING (TO BE REMOVED)	---		
BRIDGE	---		



NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CLARK COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4"x24" IRON REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NORTHWEST REGION.

EXISTING HIGHWAY RIGHT OF WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
EXISTING RIGHT OF WAY FOR STH 98: STATE PROJECT NO. 6240.



NOT TO SCALE

SW - SW SEC. 8 T26N R1E

PROJECT LOCATION

BRANCH OF ROCK CREEK

TOWN OF SHERMAN

TRANSPORTATION PROJECT PLAT NO: 7040-00-20-4.01

THAT PART OF THE SOUTHWEST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 8 AND PART OF THE NORTHWEST 1/4 OF THE NORTHWEST 1/4 OF SECTION 17, ALL IN TOWNSHIP 26 NORTH, RANGE 1 EAST, TOWN OF SHERMAN, CLARK COUNTY, WISCONSIN

RELOCATION ORDER GREENWOOD-SPENCER BRANCH ROCK CREEK CULVERT, C-10-0130 STH 98, CLARK COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

UTILITY INTERESTS TABLE			
UTILITY NUMBER	PARCEL	INTEREST DESCRIPTION	RECORDING INFORMATION
GENERAL TELEPHONE COMPANY OF WISCONSIN	2	EASEMENT	VOL 265, PG 329 DOC# 332634

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	UTILITY OWNERS	INTEREST REQUIRED
50	FRONTIER COMMUNICATIONS	RELEASE OF RIGHTS

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTEREST TO THE DEPARTMENT

SCHEDULE OF LANDS & INTERESTS REQUIRED						
PARCEL NUMBER	OWNERS	INTEREST REQUIRED	R/W NEW SF	R/W EXISTING SF	R/W TOTAL SF	TLE SF
1	LARRY LUCHT	FEE/TLE	578	3133	3711	3686
2	ROGER RUETH, CHRISTOPHER RUETH AND ROBIN RUETH	FEE/TLE	822	4567	5389	7963

FEE STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
100	496+73.00	54.83'	416427.384	731996.065
101	497+48.00	100.00'	416473.927	732070.223
102	498+18.00	100.00'	416475.213	732140.211
103	498+93.00	54.89'	416431.491	732216.027
104	498+93.00	55.11'	416321.509	732218.048
105	498+18.00	100.00'	416275.247	732143.886
106	497+48.00	100.00'	416273.961	732073.898
107	496+73.00	55.17'	416317.402	731998.087
108	497+48.00	54.85'	416428.784	732071.052
109	497+48.16	65.00'	416438.936	732071.030
110	498+17.87	65.00'	416440.217	732140.721
111	498+18.00	54.87'	416430.091	732141.040
112	497+48.00	55.15'	416318.802	732073.073
113	498+18.00	55.13'	416320.109	732143.061

TLE STATION & OFFSET TABLE		
POINT	STATION	OFFSET
101	497+48.00	100.00'
102	498+18.00	100.00'
106	497+48.00	100.00'
107	496+73.00	55.17'



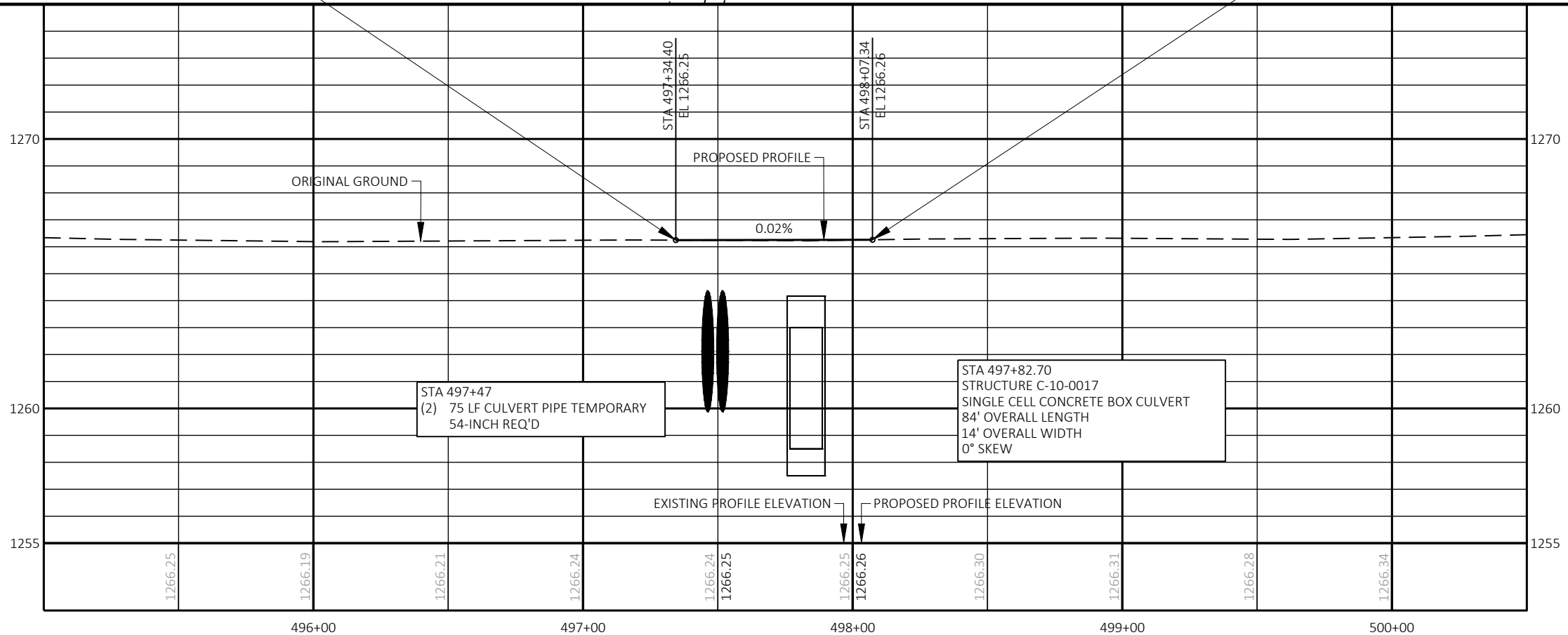
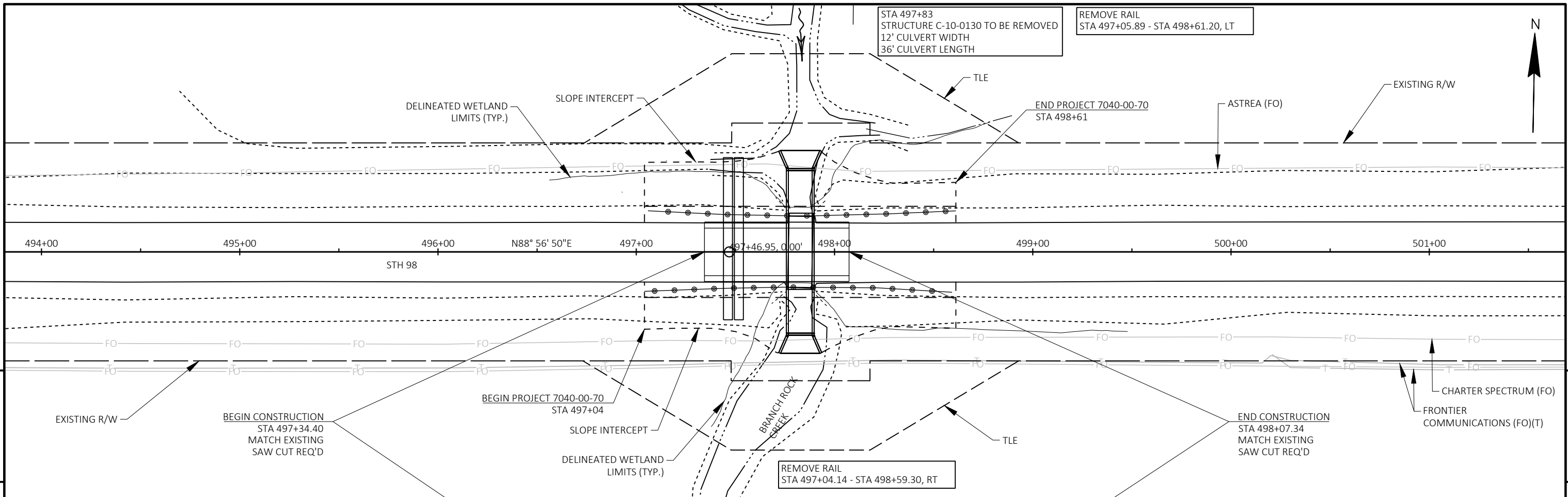
SIGNATURE: *Jason L. Cance* DATE: 5-22-2019
PRINT NAME: JASON L. CANCE
REGISTRATION NUMBER: S-2688
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION NORTHWEST REGION - EAU CLAIRE OFFICE
SIGNATURE: *Debra B. Stensland* DATE: 5-22-2019
PRINT NAME: DEBRA B. STENSLAND

CONVENTIONAL ABBREVIATIONS			
ACCESS RIGHTS	AR	OUTLOT	OL
ACRES	AC	PAGE	P
AHEAD	AH	POINT OF TANGENCY	PT
ALUMINUM	ALUM	PROPERTY LINE	PL
AND OTHERS	ET AL	RECORDED AS	(100')
BACK	BK	REEL / IMAGE	R/I
BLOCK	BLK	REFERENCE LINE	R/L
CENTERLINE	C/L	PERMANENT LIMITED EASEMENT	PLE
CERTIFIED SURVEY MAP	CSM	POINT OF BEGINNING	POB
CONCRETE	CONC	POINT OF CURVATURE	PC
COUNTY	CO	POINT OF COMPOUND CURVE	PCC
COUNTY TRUNK HIGHWAY	CTH	POINT OF INTERSECTION	PI
DISTANCE	DIST	REMAINING	REM
CORNER	COR	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
DOCUMENT NUMBER	DOC	RIGHT	RT
EASEMENT	EASE	RIGHT OF WAY	R/W
EXISTING	EX	SECTION	SEC
GAS VALVE	GV	SEPTIC VENT	SEPV
GRID NORTH	GN	SQUARE FEET	SF
HIGHWAY EASEMENT	HE	STATE TRUNK HIGHWAY	STH
IDENTIFICATION	ID	STATION	STA
LAND CONTRACT	LC	TELEPHONE PEDESTAL	TPE
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT PLAT	TPP
NATIONAL GEODETIC SURVEY	NGS	UNITED STATES HIGHWAY	USH
NUMBER	NO	VOLUME	V

CONVENTIONAL UTILITY SYMBOLS

WATER	---	NON-COMPENSABLE	---
GAS	---	COMPENSABLE	---
TELEPHONE	---		
OVERHEAD TRANSMISSION LINES	---		
ELECTRIC	---		
CABLE TELEVISION	---		
FIBER OPTIC	---		
SANITARY SEWER	---		
STORM SEWER	---		
ELECTRIC TOWER	---		
POWER POLE	---		
TELEPHONE POLE	---		
TELEPHONE PEDESTAL	---		

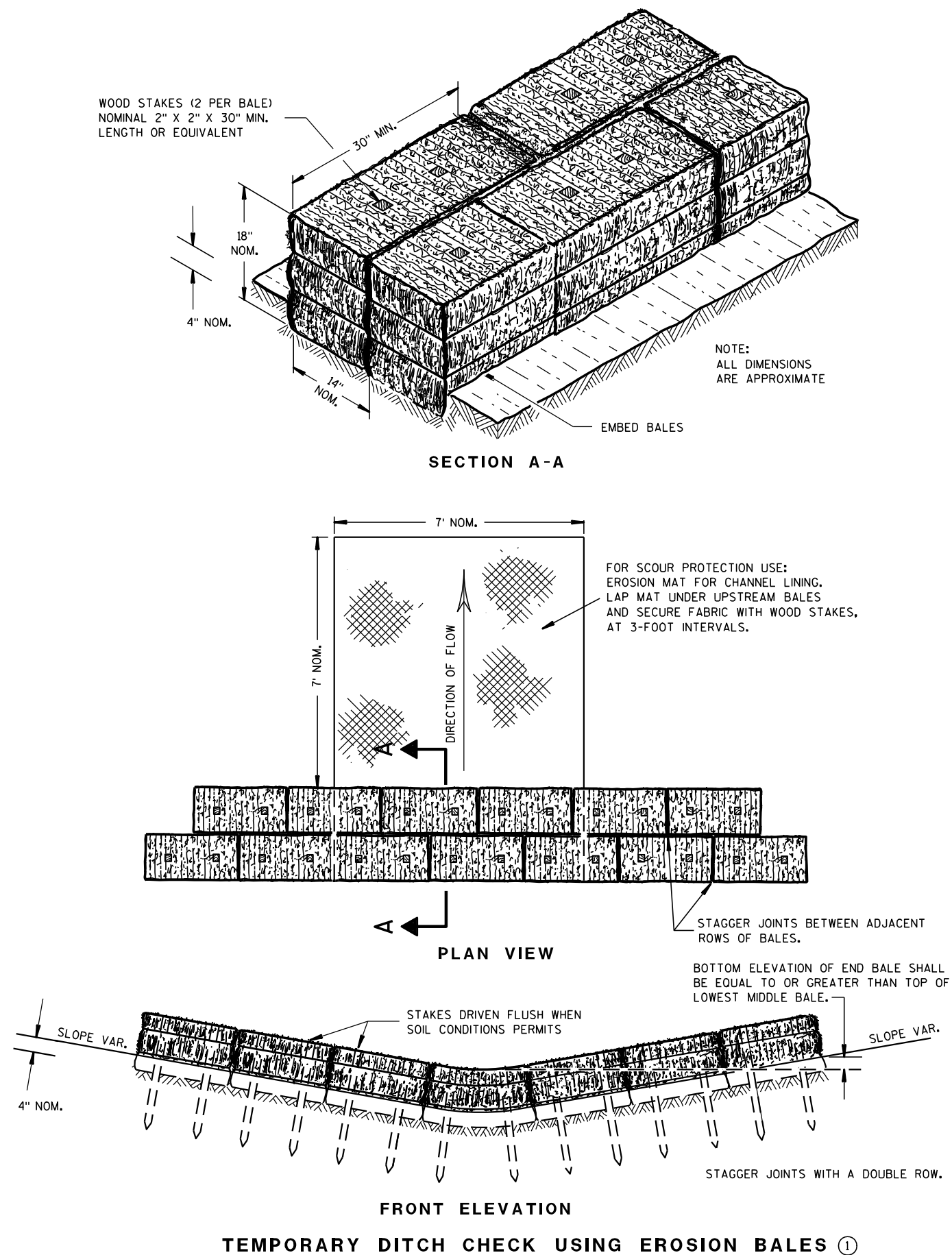
COURSE	BEARING	DISTANCE
124-125	N01° 03' 10"W	0.17'
125-100	N01° 03' 10"W	54.83'
100-108	N88° 55' 49"E	75.00'
108-109	N00° 07' 23"W	10.15'
109-110	N88° 56' 50"E	69.70'
110-111	S01° 48' 16"E	10.13'
111-103	N88° 55' 49"E	75.00'
103-127	S01° 03' 10"E	54.89'
127-126	S01° 03' 10"E	0.11'
126-104	S01° 03' 10"E	55.00'
104-113	S88° 55' 49"W	75.00'
113-114	S01° 03' 10"E	9.87'
114-115	S88° 56' 50"W	70.00'
115-112	N01° 03' 10"W	9.85'
112-107	S88° 55' 49"W	75.00'
107-124	N01° 03' 10"W	55.00'



PROJECT NO: 7040-00-70	HWY: STH 98	COUNTY: CLARK	PLAN AND PROFILE: PLAN & PROFILE	SHEET E
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Standard Detail Drawing List

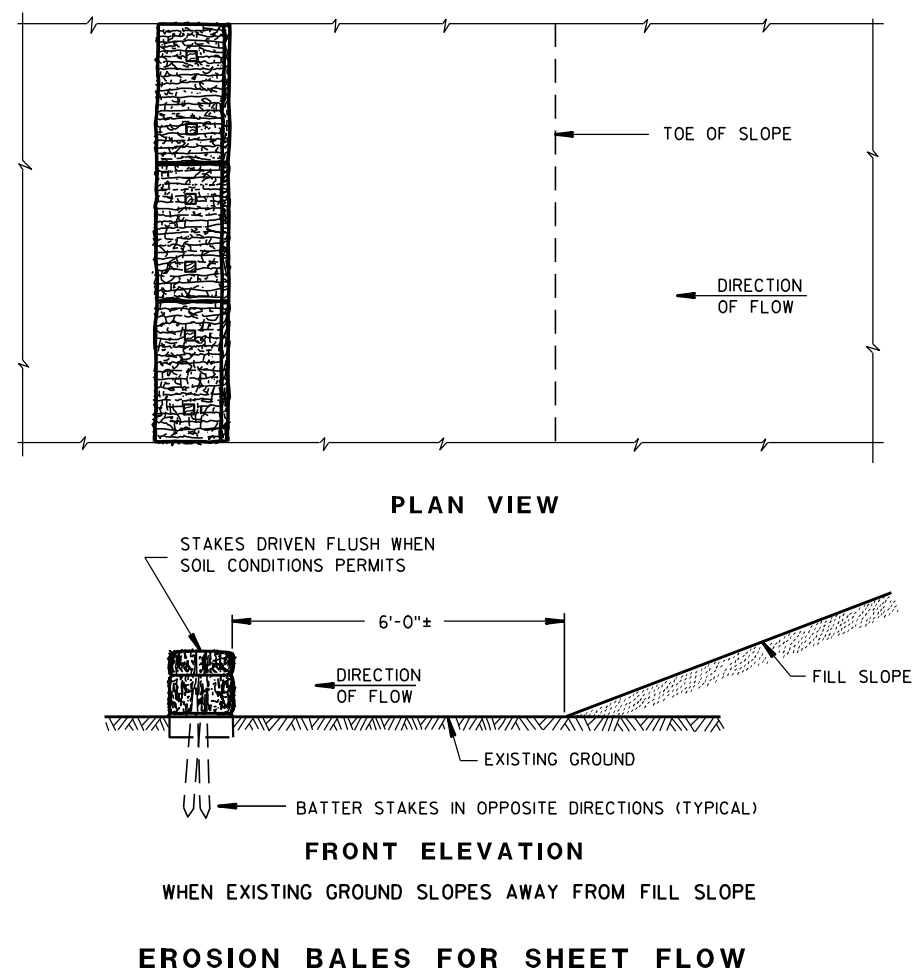
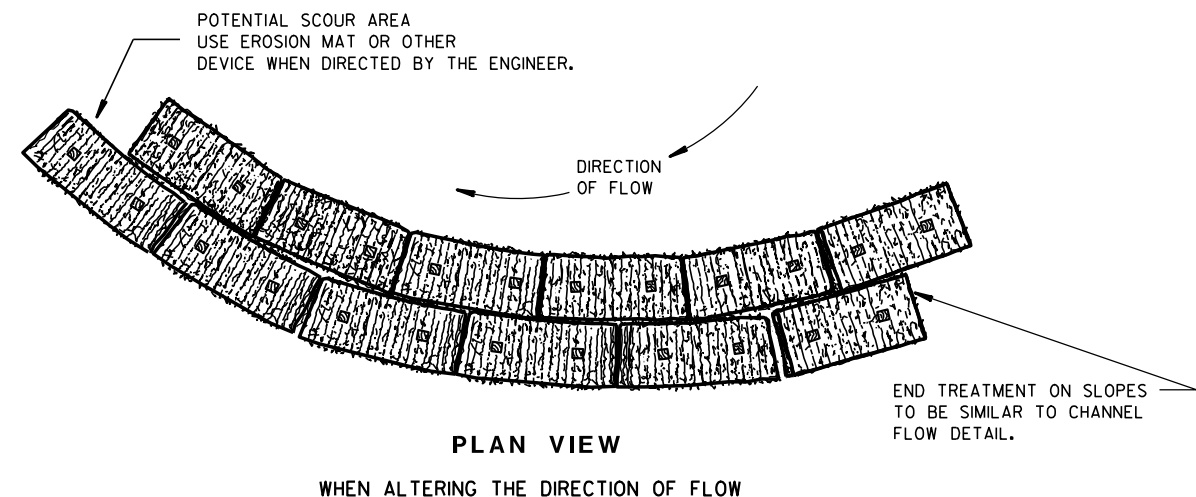
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
09G02-05A	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
09G02-05B	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
09G02-05C	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
12A03-10	NAME PLATE (STRUCTURES)
14B07-15A	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15E	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
15C02-07F	ADVANCED WIDTH RESTRICTION SIGNING
15C11-07A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15D33-06	TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

TYPICAL INSTALLATIONS OF
EROSION BALES / TEMPORARY
DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

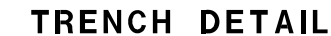
6/04/02
DATE

FHWA

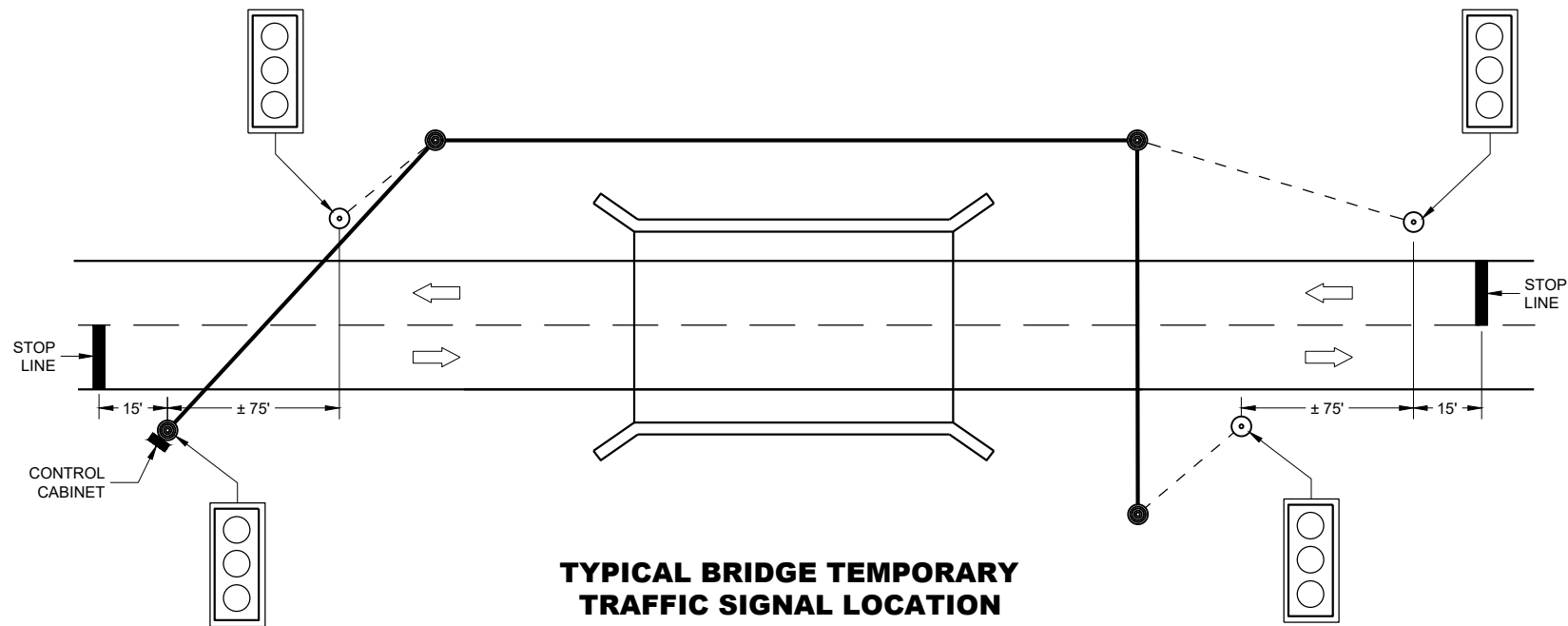
/S/ Beth Canestra
CHIEF ROADWAY DEVELOPMENT ENGINEER



- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED <u>4-29-05</u> DATE	<u>/S/ Beth Cannestra</u> CHIEF ROADWAY DEVELOPMENT ENGINEER



TYPICAL BRIDGE TEMPORARY TRAFFIC SIGNAL LOCATION

LEGEND

- WOOD POLE (NON-BREAKAWAY)
- WOOD POST (BREAKAWAY)
- SIGNAL CABLE
- SIGNAL CABLE W/MESSENGER
- DIRECTION OF TRAFFIC
- LED TRAFFIC SIGNAL WITH BACKPLATE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

POLE MOUNTED TRAFFIC SIGNAL CONTROL CABINET MAY BE MOUNTED ON THE SERVICE POLE IF THE ELECTRICAL UTILITY ALLOWS THE INSTALLATION.

WHEN UTILITY POLES ARE USED TO SPAN THE TEMPORARY OVERHEAD CABLE, WRITTEN PERMISSION MUST BE OBTAINED FROM THE OWNER OF THE POLES AND GIVEN TO THE PROJECT MANAGER. ALL PERTINENT UTILITY AND CODE CLEARANCES SHALL BE MAINTAINED.

WOOD POLES (NON-BREAKAWAY) SHALL BE NO CLOSER TO EDGE OF PAVEMENT THAN OFFSET DISTANCE CHART ALLOWS OR 4 FEET BEHIND PROTECTIVE BARRIER (BEAM GUARD, ETC.).

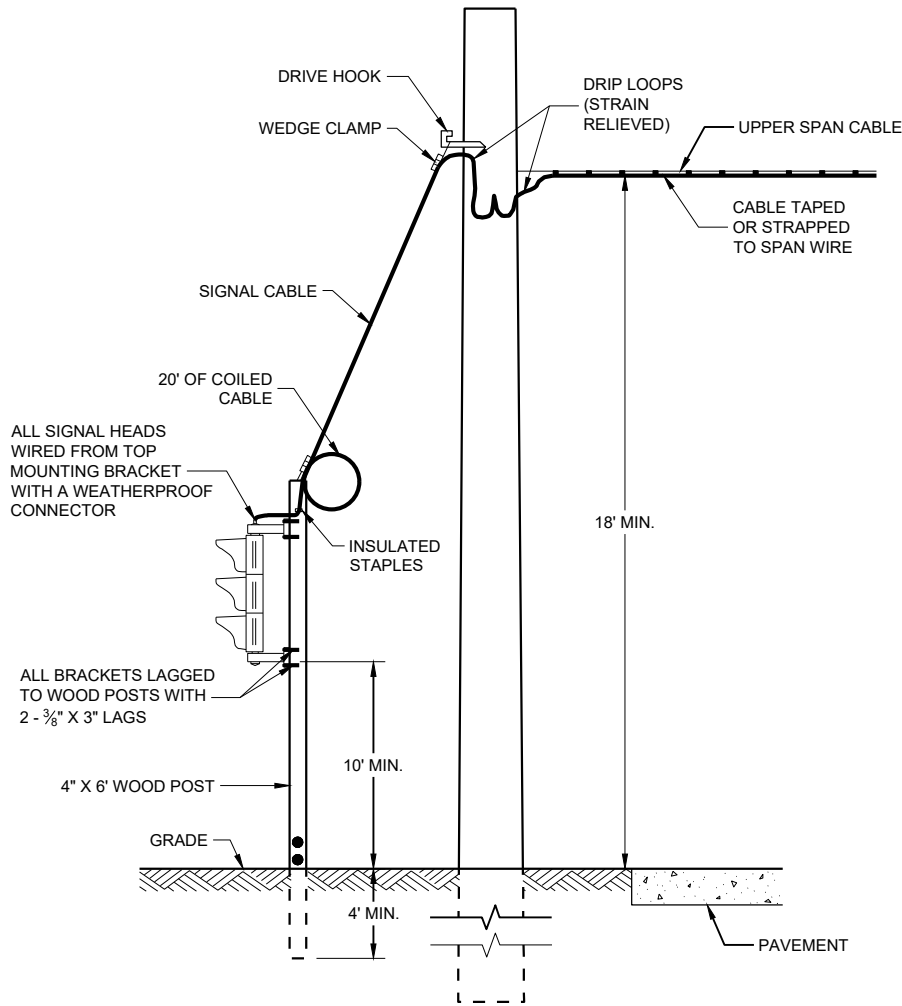
WOOD POSTS (BREAKAWAY) SHALL BE NO CLOSER THAN 2 FEET OUTSIDE OF SHOULDER.

VERTICAL CLEARANCE ETC. PER NEC.

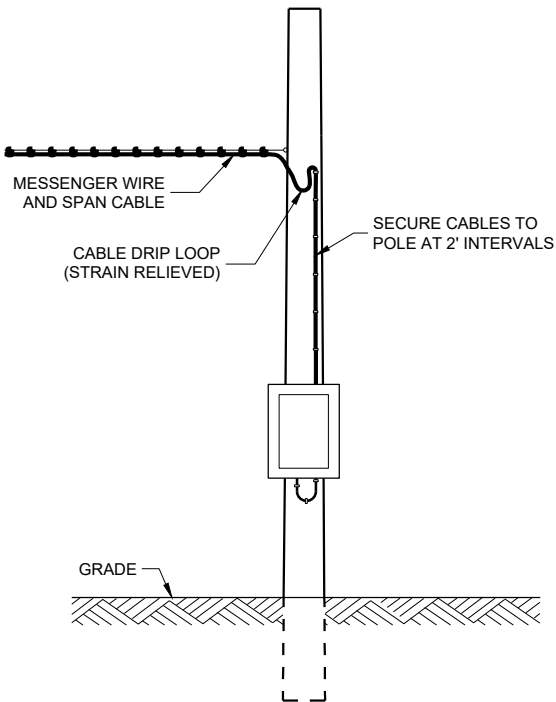
TRAFFIC SIGNAL FACES SHALL BE TYPICALLY PLACED 12 FEET FROM EDGE OF PAVEMENT.

EACH TRAFFIC SIGNAL SHALL HAVE A BACKPLATE.

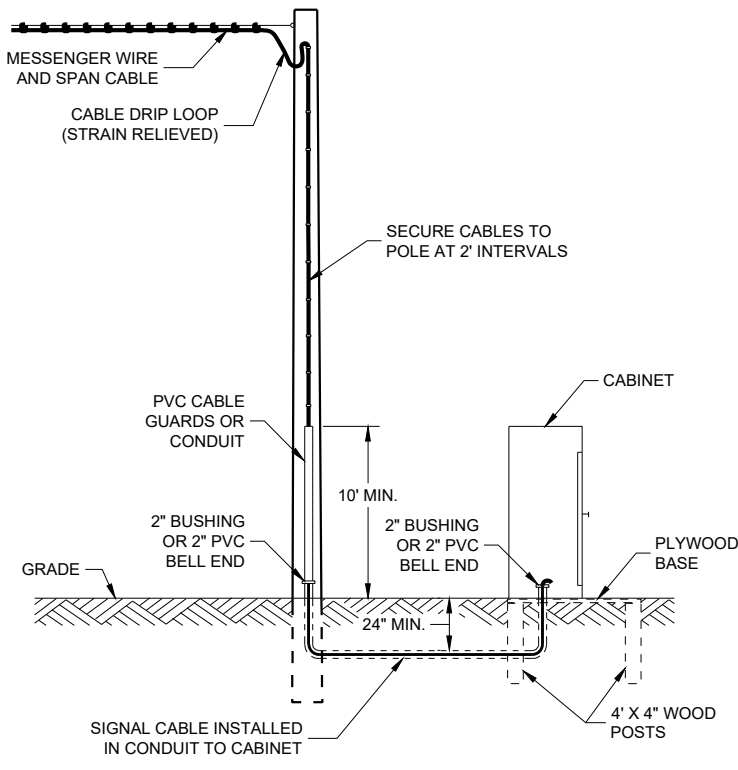
SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15D33.



TYPICAL DROP TO TRAFFIC SIGNAL FACE



POLE MOUNT CABINET INSTALLATION



GROUND MOUNT CABINET INSTALLATION

MINIMUM POLE LENGTHS	CLASS	POLE BURIAL DEPTHS
25'	V	5'
30'	V	6'
35'	IV	7'
40'	IV	8'
45'	IV	9'

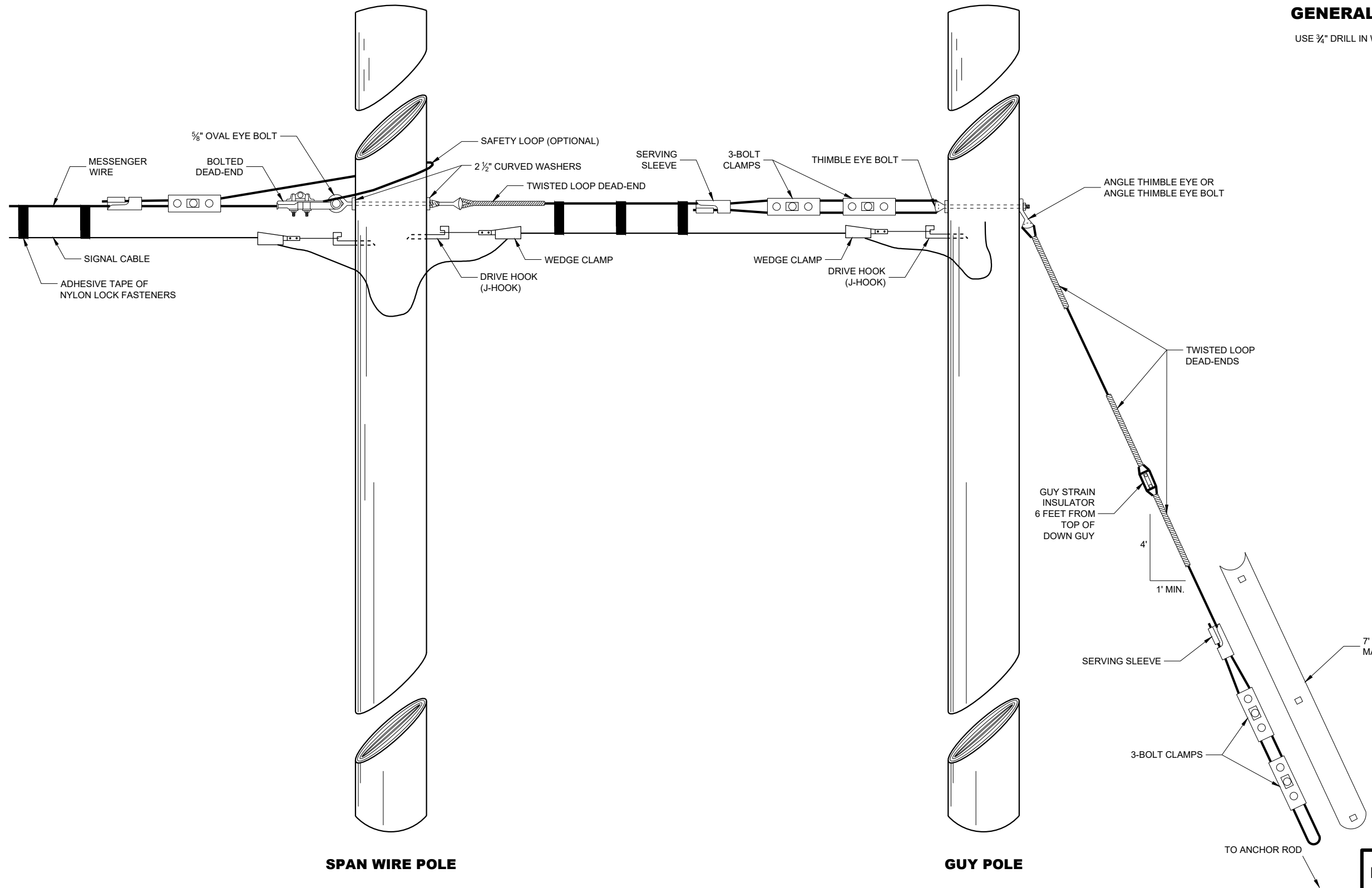
OFFSET DISTANCES FOR TEMPORARY NON-BREAKAWAY POLES	
SPEED LIMIT	OFFSET DISTANCE*
GREATER THAN 45 MPH	18 FT
45 MPH OR LESS	12 FT
45 MPH OR LESS W/CURBS	2 FT

* NOTE: OFFSET MEASURED FROM OUTER EDGE OF OUTSIDE THRU LANE.

BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Ahmet Demirelek
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA ENGINEER

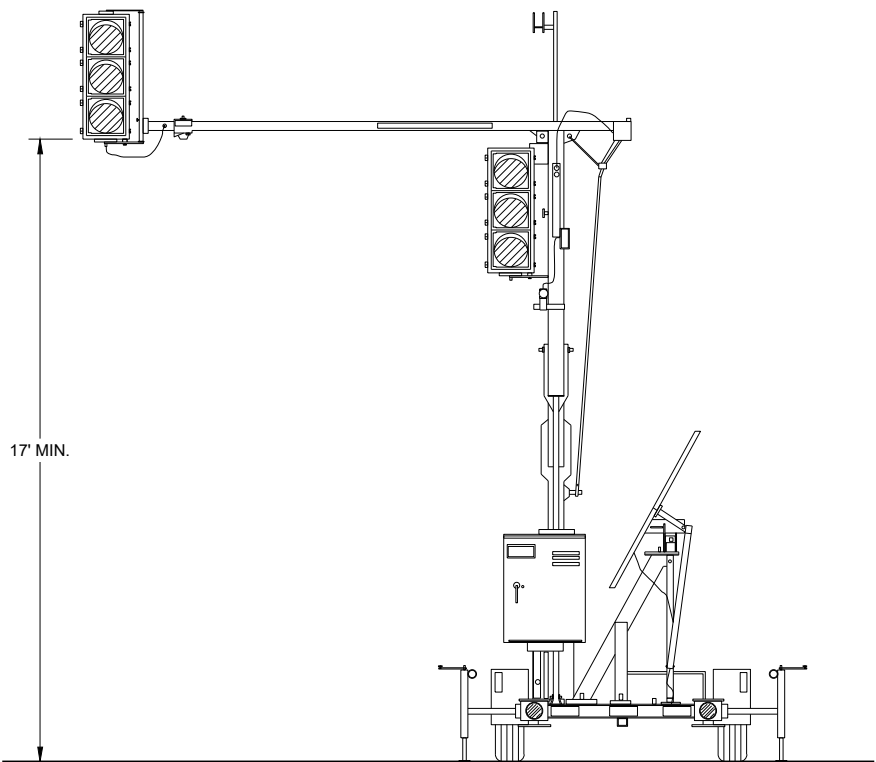


GENERAL NOTES

USE 3/4" DRILL IN WOOD POLE TO PROVIDE FOR 5/8" BOLTS.

TYPICAL DEAD-ENDINGS OR GUYING

BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2015 DATE	/S/ Ahmet Demerbilek ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	

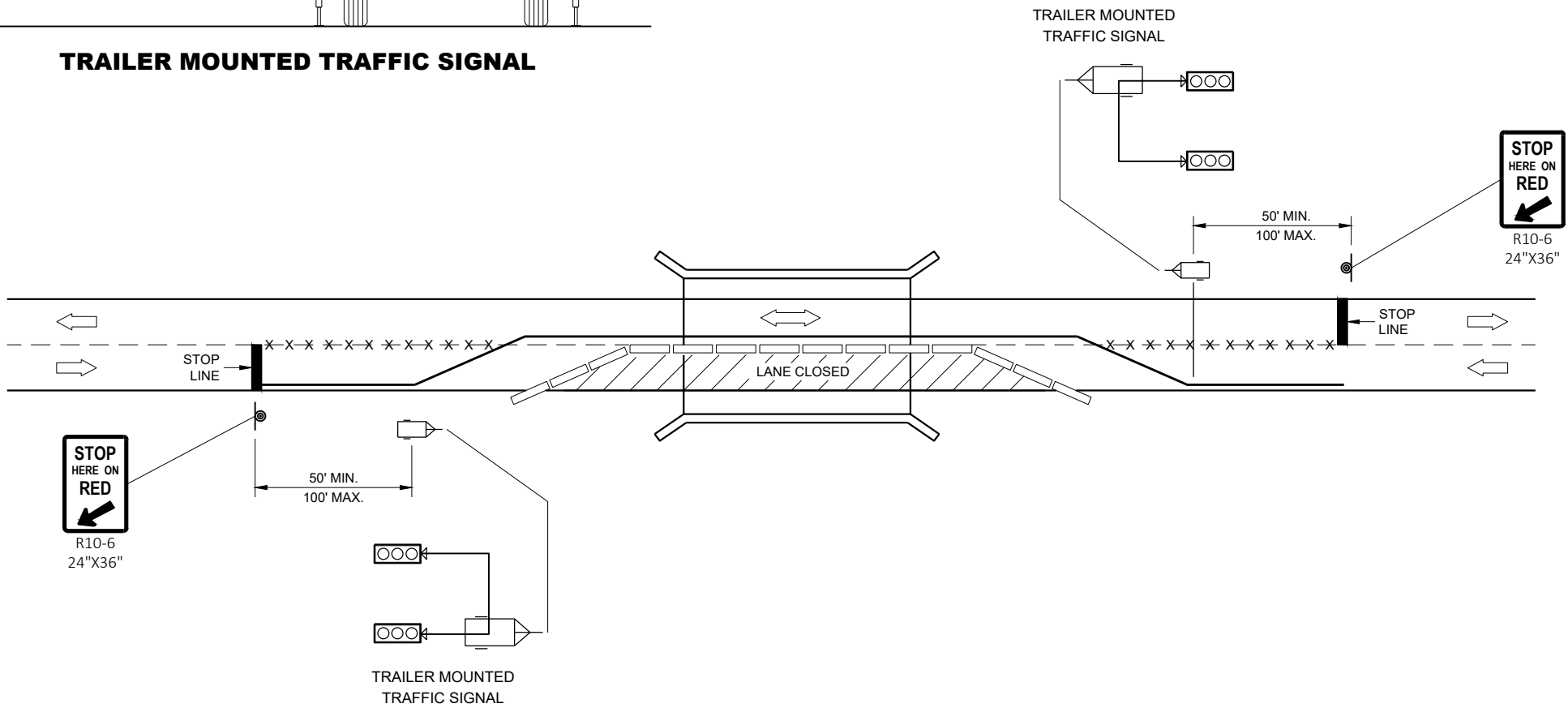


TRAILER MOUNTED TRAFFIC SIGNAL

GENERAL NOTES

DETAIL OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15D33.



TYPICAL TRAILER MOUNTED TRAFFIC SIGNAL LOCATION

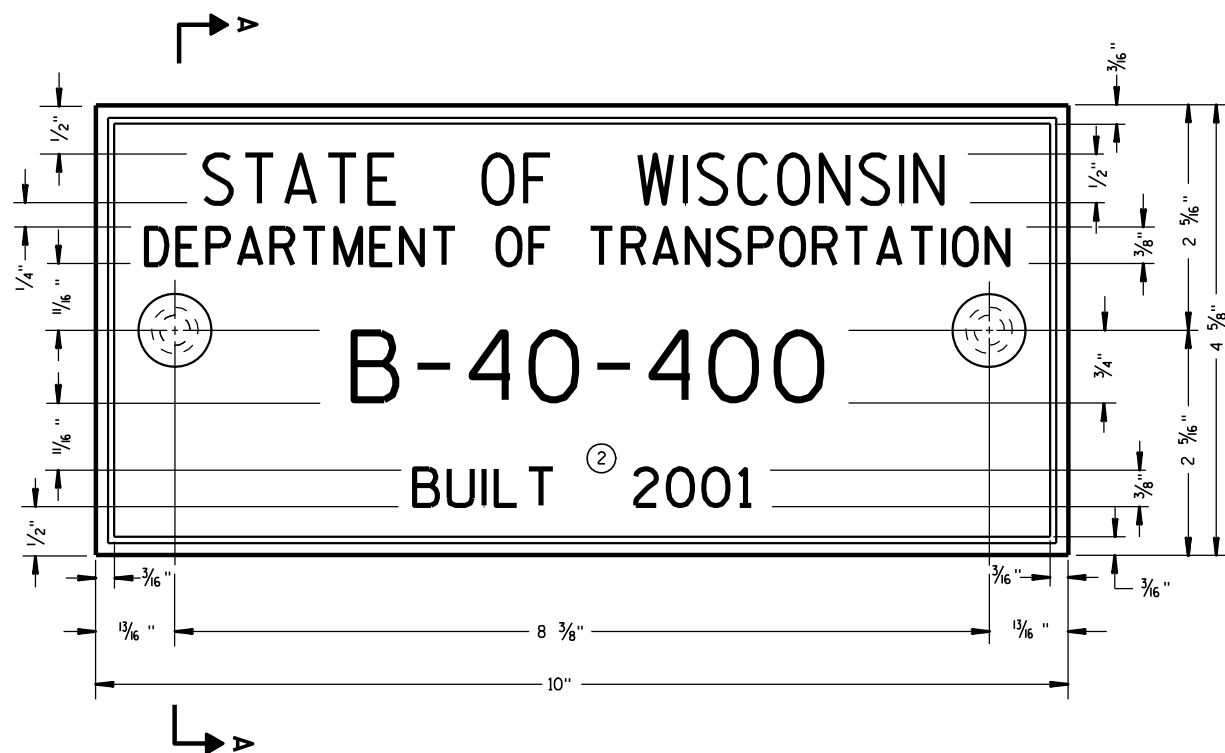
LEGEND

- POST MOUNTED SIGN
- TEMPORARY PRECAST CONCRETE BARRIER
- TRAILER MOUNTED TRAFFIC SIGNAL
- REMOVE PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC

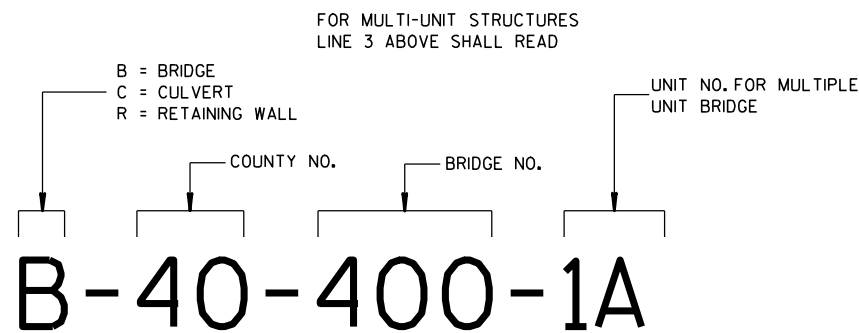
BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2015
DATE /S/ Ahmet Demerbilek
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)



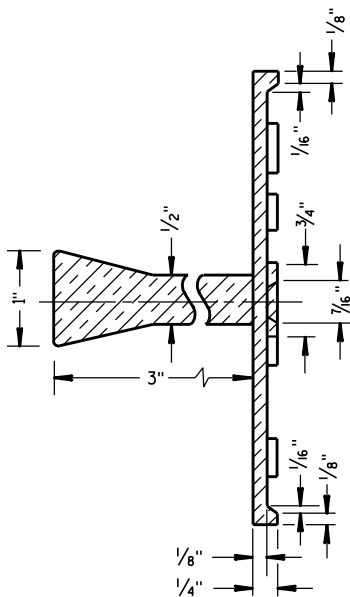
NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES

GENERAL NOTES

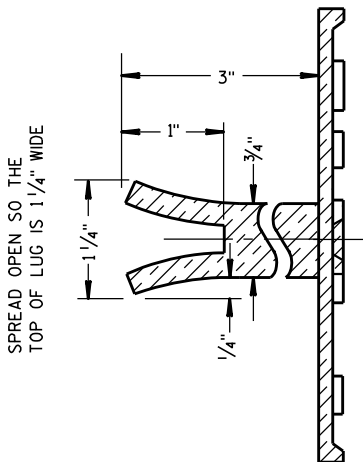
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

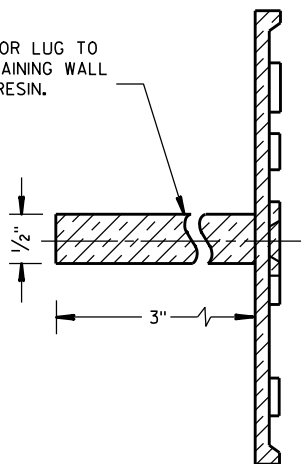


SECTION A-A



ALTERNATE LUG

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

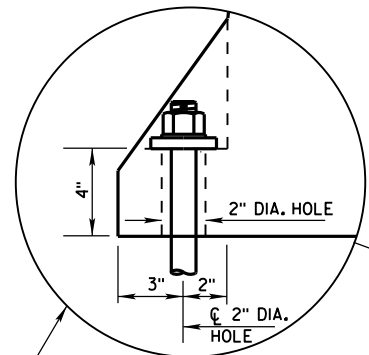


ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

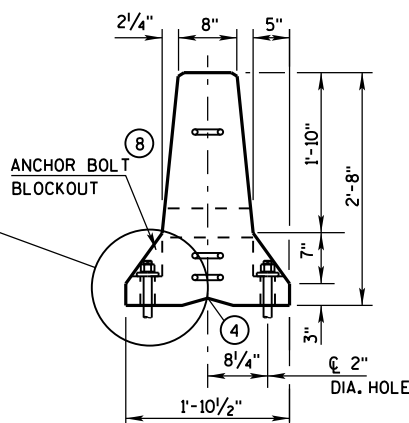
NAME PLATE
(STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

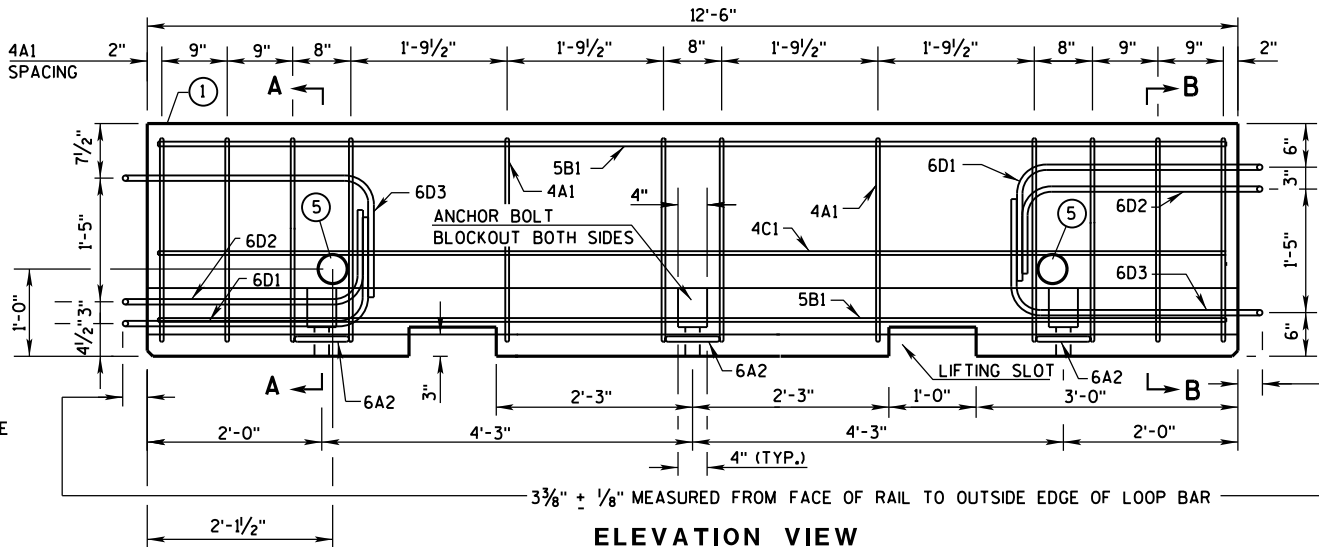
APPROVED
3/26/10
DATE
/S/ Scot Becker
CHIEF STRUCTURAL DEVELOPMENT ENGINEER
FHWA



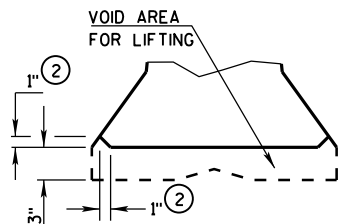
ANCHOR ON TRAFFIC SIDE
ONLY WHEN REQUIRED
(SEE SHEET D FOR ADDITIONAL
ANCHOR DETAIL)



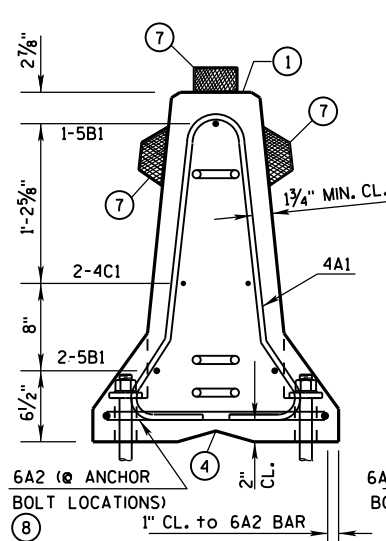
END VIEW



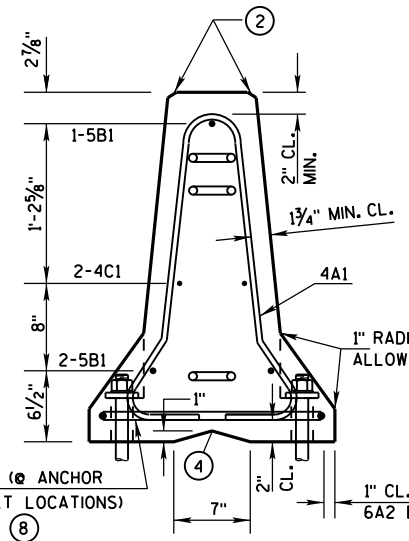
ELEVATION VIEW



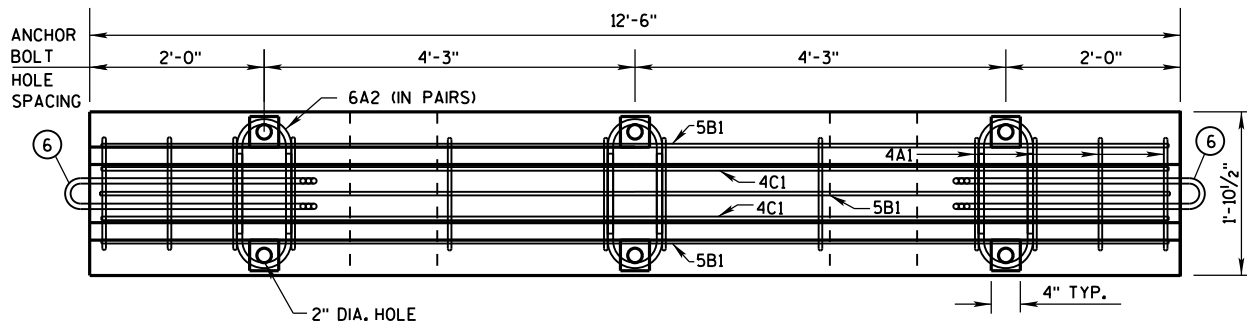
DETAIL "B"
LIFTING SLOT DETAIL



SECTION A-A
(STIRRUP PLACEMENT)

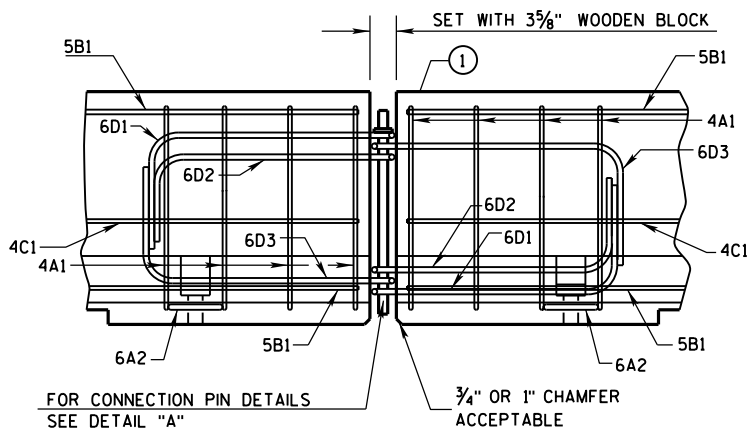


SECTION B-B
(STIRRUP PLACEMENT)

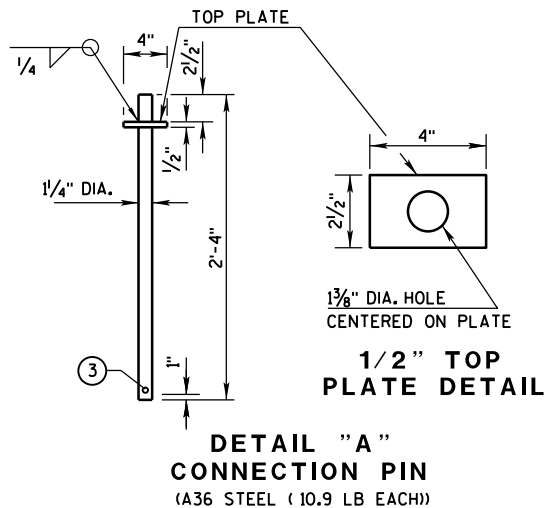


PLAN VIEW

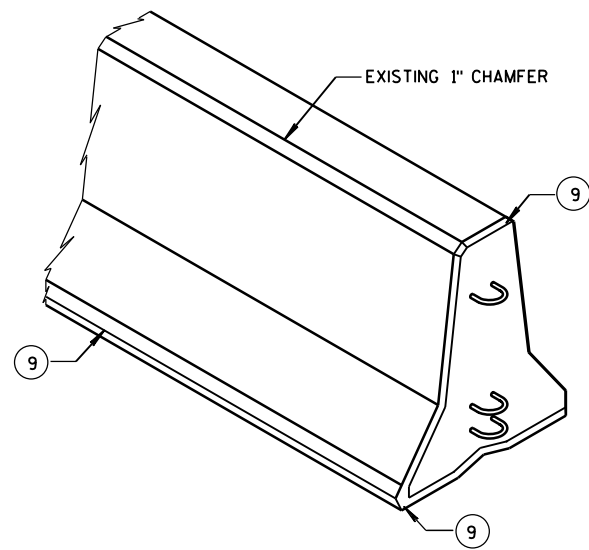
DETAILS OF BARRIER SECTION



DETAILS OF BARRIER CONNECTION



DETAIL "A"
CONNECTION PIN
(A36 STEEL (10.9 LB EACH))



GENERAL NOTES

THESE GENERAL NOTES APPLY TO SHEETS 14B7-15(a) THRU 14B7-15(i).

DO NOT INTERMIX CONCRETE BARRIER TEMPORARY PRECAST, 12'-6" (CBTP12.5) WITH OTHER TEMPORARY CONCRETE BARRIERS.

USE ASTM A-615, GRADE 60, DEFORMED STEEL BARS FOR BARS 4A1, 6A2, 5B1 AND 4C1 IN THE BARRIER SECTION AND FOR 4V1, 4V2, 4V3, 4V4, 4V5, 4V6, 4F1, 4F2 AND 5F3 IN THE BARRIER TAPER SECTION.

LOOP BARS 6D1, 6D2 AND 6D3 SHALL BE 3/4" SMOOTH STEEL BARS WITH A MINIMUM YIELD STRENGTH OF 60 KSI, A TENSILE STRENGTH OF NOT LESS THAN 1.25 TIMES THE YIELD STRENGTH BUT A MINIMUM OF 80 KSI, A MINIMUM 14% ELONGATION IN 8 INCHES AND PASSING A 180 DEGREE BEND TEST USING A 3-1/2" PIN BEND DIAMETER FOR BEND TESTS. THE LOOPS SHALL BE INSTALLED WITHIN 1/8" OF THE PLAN DIMENSION.

CONSTRUCT LIFTING SLOTS AS SPECIFIED ON THE PLANS TO FACILITATE THE DRAINAGE OF WATER AFTER INSTALLATION.

PLACE BARRIER ON A PAVED SURFACE. REMOVE ALL LOOSE DIRT AND SAND FROM THE ROADWAY SURFACE PRIOR TO PLACEMENT OF THE BARRIER.

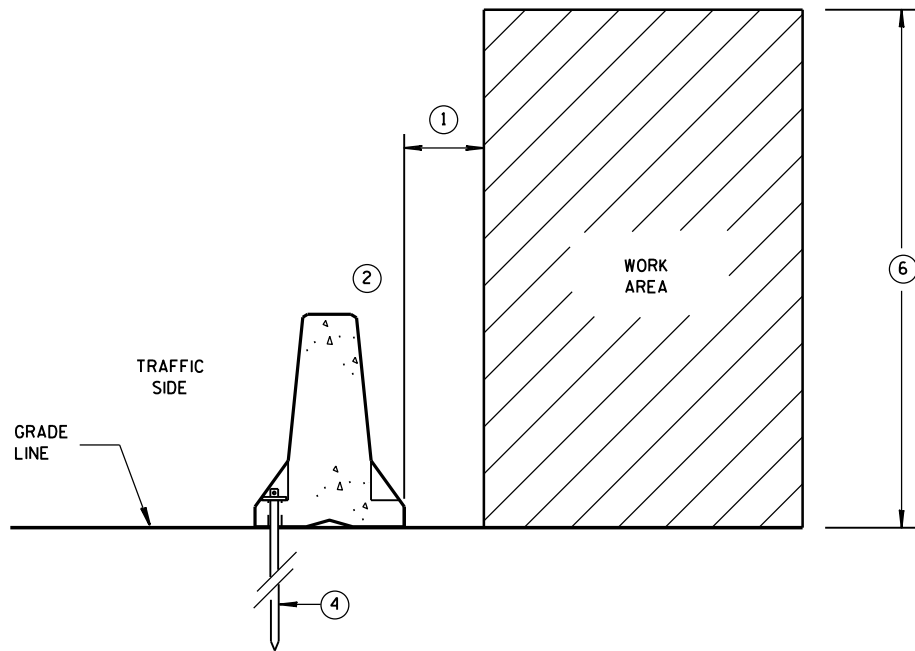
INSTALL MECHANICAL OR ADHESIVE ANCHORS PER MANUFACTURER'S RECOMMENDATIONS. PROVIDE MANUFACTURER'S INFORMATION TO PROJECT ENGINEER.

- MARK ONE END OF EACH BARRIER PERMANENTLY BY FORMING INTO THE BARRIER THE FOLLOWING INFORMATION:
 - TYPE: WICBTP
 - MANUFACTURER
 - DATE MANUFACTURED (MONTH AND YEAR)
- 1" CHAMFER TO PREVENT SPALLING.
- A 3/8" HOLE IN THE CONNECTION PIN, AT THE LOCATION SHOWN, IS ACCEPTABLE, BUT NOT REQUIRED..
- "V" NOTCH IS OPTIONAL.
- THE 4" DIAMETER, 11 GAUGE STEEL, ROUND MECHANICAL TUBING SLEEVE FOR LIFTING (OPTIONAL).
- NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.
- USE DELINEATORS CONFORMING TO SECTION 633 OF THE STANDARD SPECIFICATIONS. CONTRACTOR MAY USE ALTERNATE SHAPES AND HOUSING. INSTALL DELINEATORS ACCORDING TO MANUFACTURES INSTRUCTION. INSTALL YELLOW REFLECTORS WHEN BARRIER IS LOCATED TO THE LEFT OF TRAFFIC AND WHITE REFLECTORS WHEN BARRIER IS LOCATED TO THE RIGHT OF TRAFFIC. SPACE DELINEATORS A MAXIMUM OF 25 FEET APART. PROVIDE TOP MOUNTED DELINEATORS IN ADDITION TO THE SIDE MOUNTED DELINEATORS ON ALL BARRIER INSTALLATIONS LOCATED ON A CURVED ALIGNMENT LONGER THAN 200 FEET AND ON BARRIERS USED TO SEPARATE OPPOSING TRAFFIC.
- SEE SHEET D FOR HOW TO ANCHOR BARRIER. SEE SHEET E FOR WHEN TO ANCHOR BARRIER.
- 1" CHAMFER OPTIONAL.

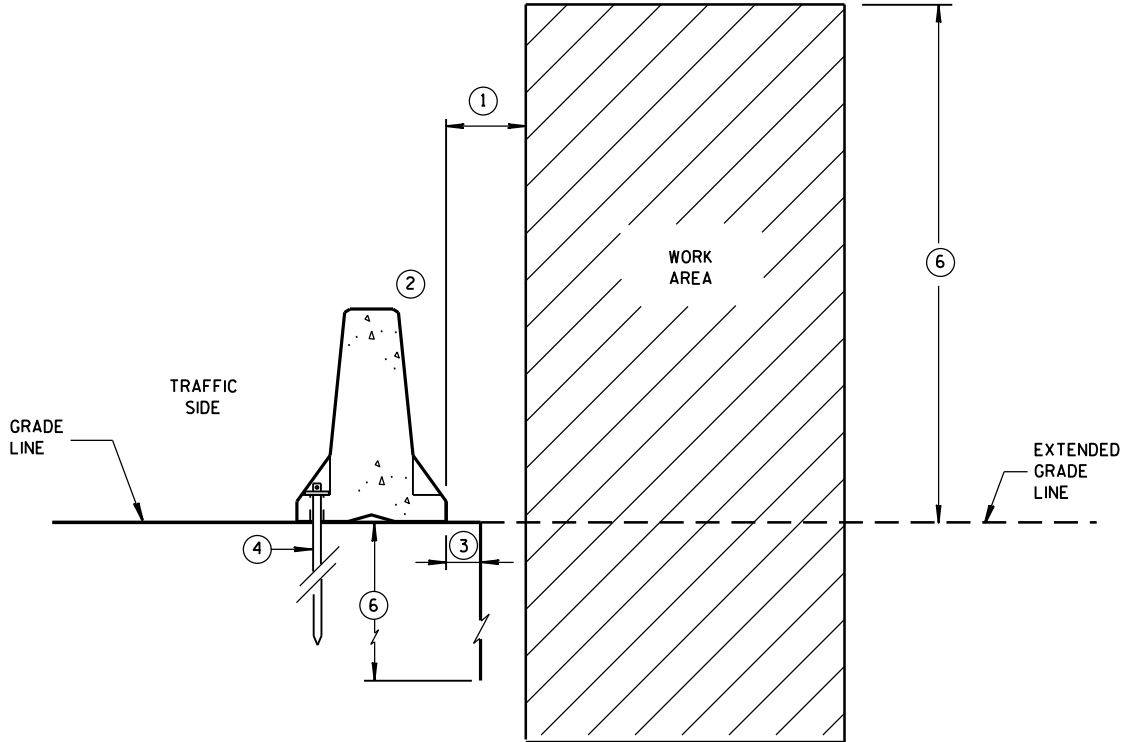
f'c = 4,000 psi

CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**ANCHORED BARRIER SPACE REQUIREMENTS
FOR HAZARDS EXTENDED
ABOVE THE GRADE LINE**

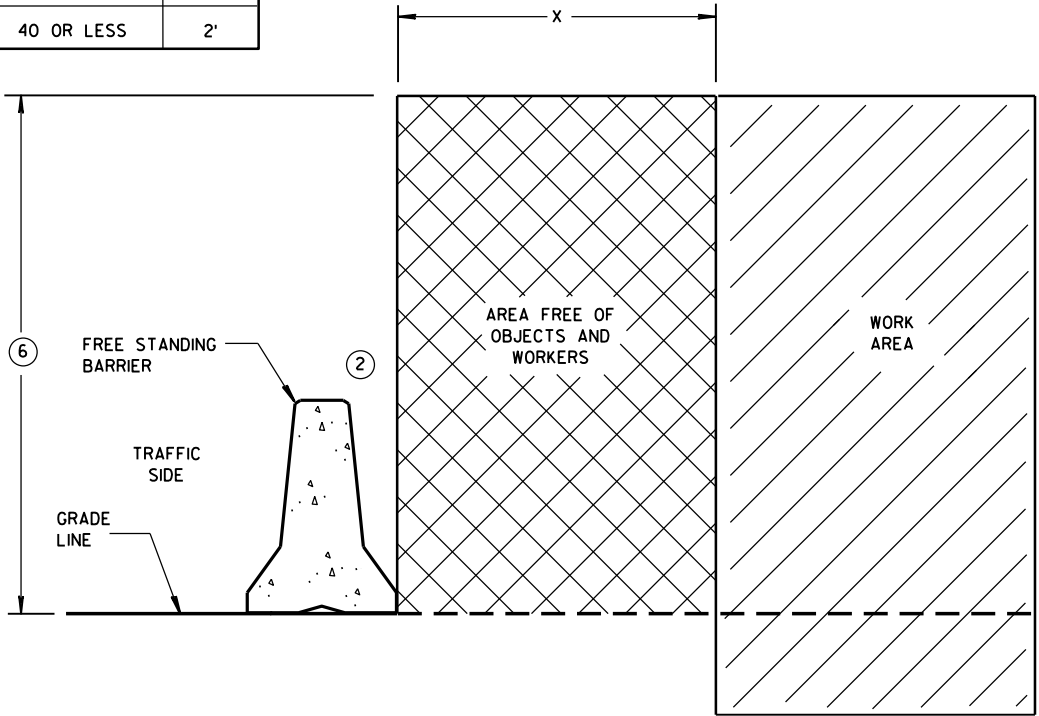


**ANCHORED BARRIER SPACE REQUIREMENTS
ON VERTICAL DROP OFFS**

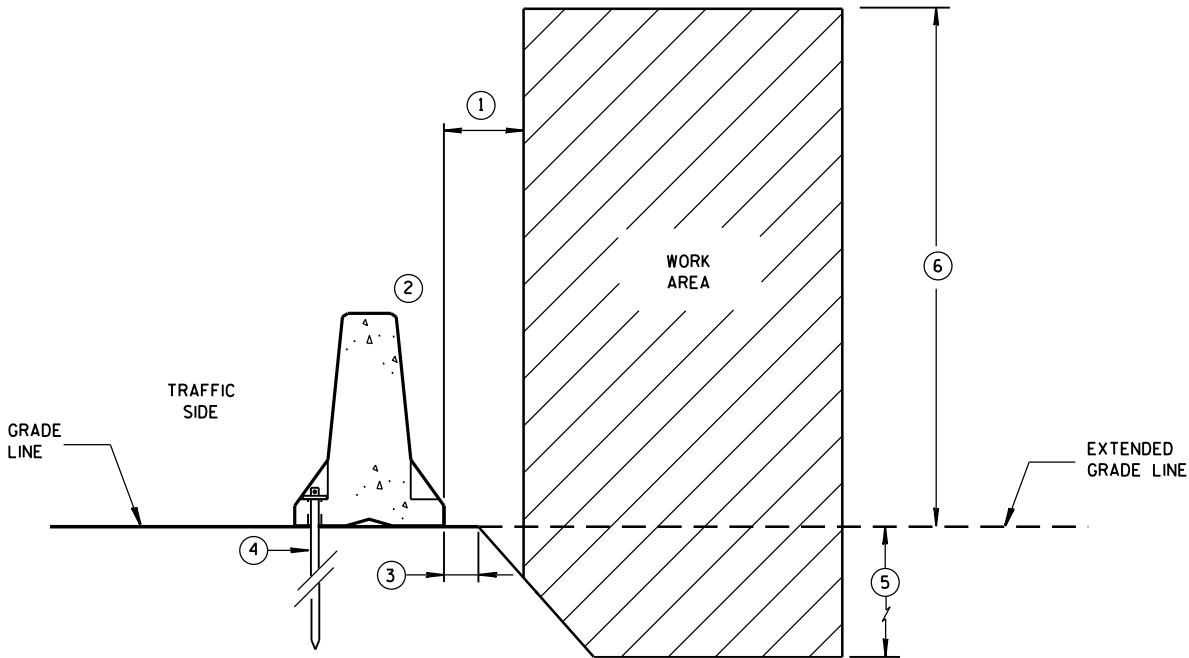
GENERAL NOTES

- ① WHEN OBJECTS EXTEND ABOVE THE GRADE, A MINIMUM OF 1 FOOT IS REQUIRED FROM BACK OF BARRIER TO OBJECT. SEE OTHER DETAILS FOR FOR THE MINIMUM OFFSET FROM BACK OF BARRIER TO SLOPES OR VERTICAL DROPS.
- ② OBJECTS ARE NOT TO BE PLACED ON, MOUNTED TO, OR LEANED AGAINST THE BARRIER WITHOUT PERMISSION OF THE PROJECT ENGINEER.
- ③ SEE OTHER DETAIL ON SHEET "D" FOR SPACE REQUIREMENTS.
- ④ SEE BOLT THROUGH DECK, REMOVABLE ADHESIVE ANCHOR, OR A STAKE DOWN FOR ASPHALTIC SURFACE TREATMENT DETAILS. ASPHALTIC ANCHOR SHOWN.
- ⑤ DEPTH OF 3 FEET OR MORE.
- ⑥ Y = 6'-6".

POSTED SPEED MPH	X
45 OR GREATER	4'
40 OR LESS	2'



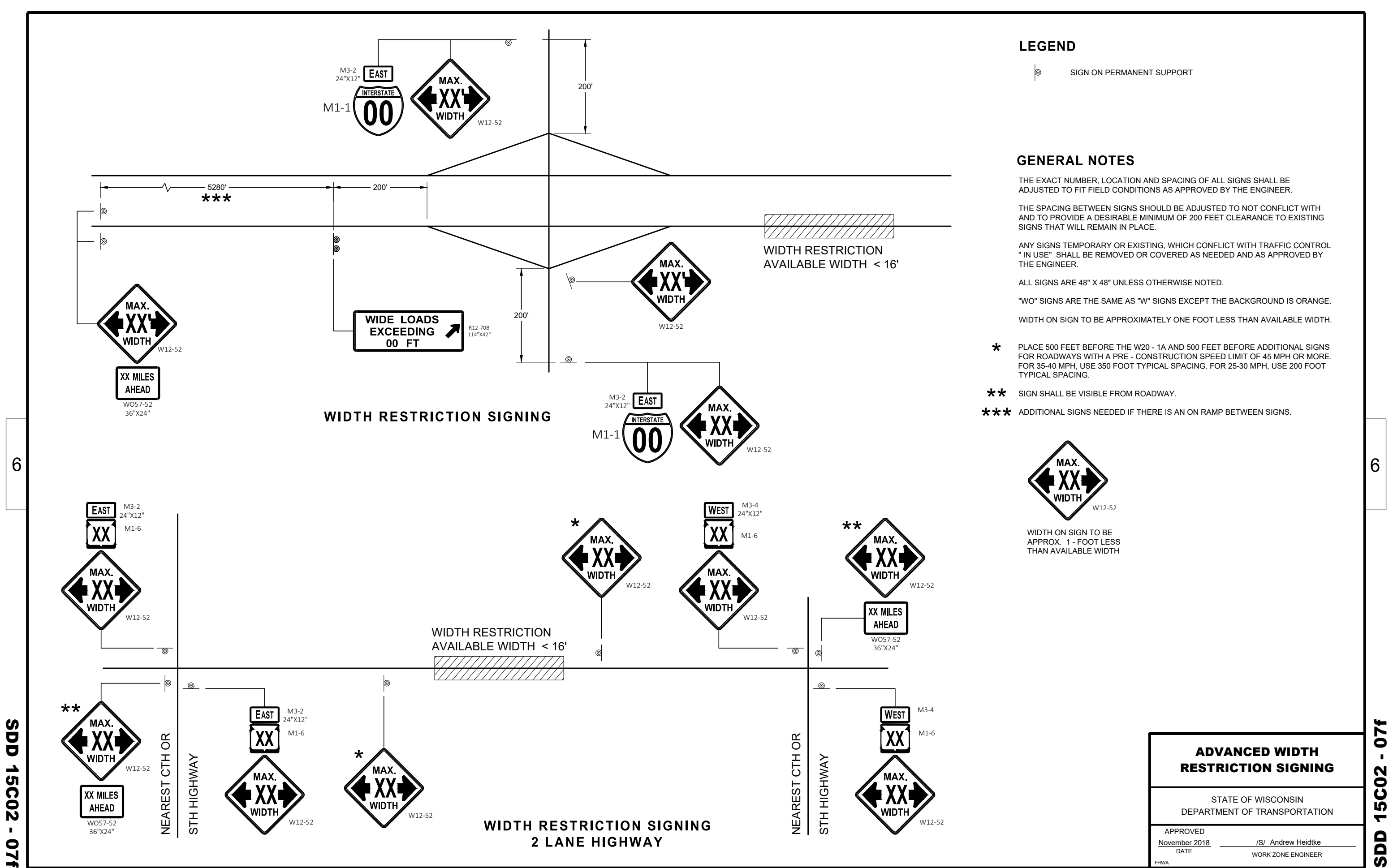
FREE STANDING BARRIER SPACE REQUIREMENTS

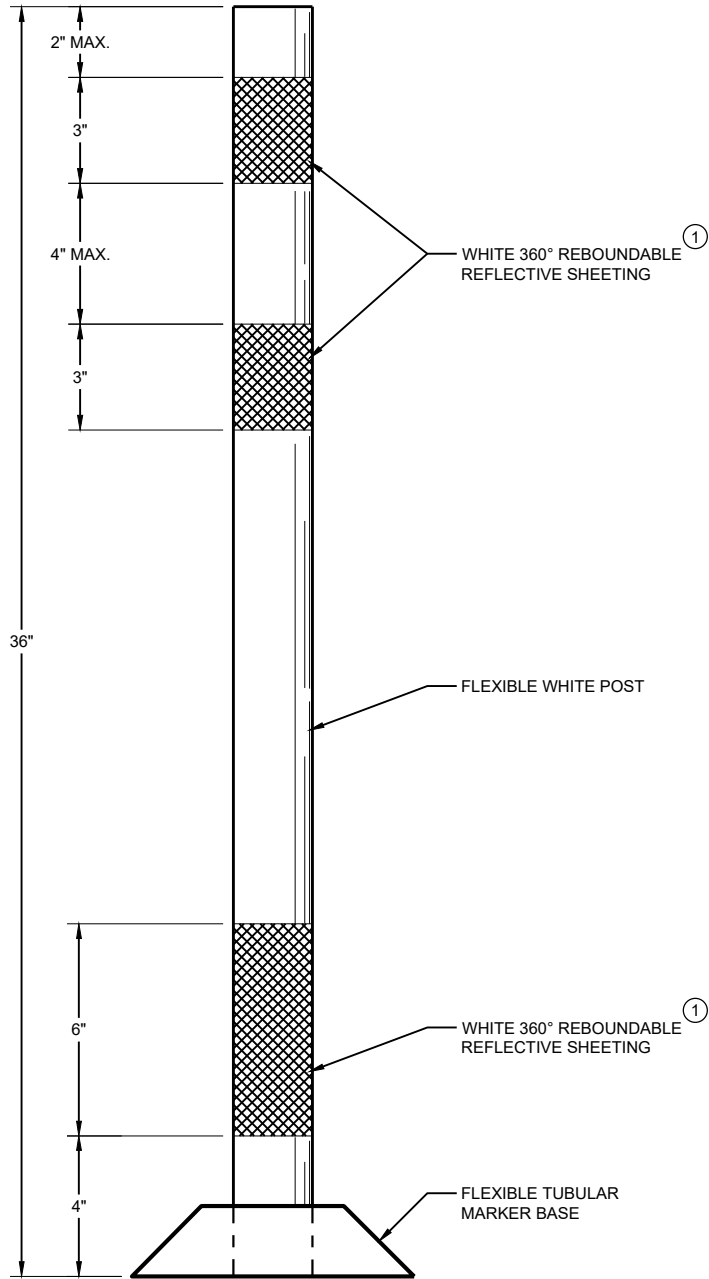


**ANCHORED BARRIER SPACE REQUIREMENTS
ON SLOPES**

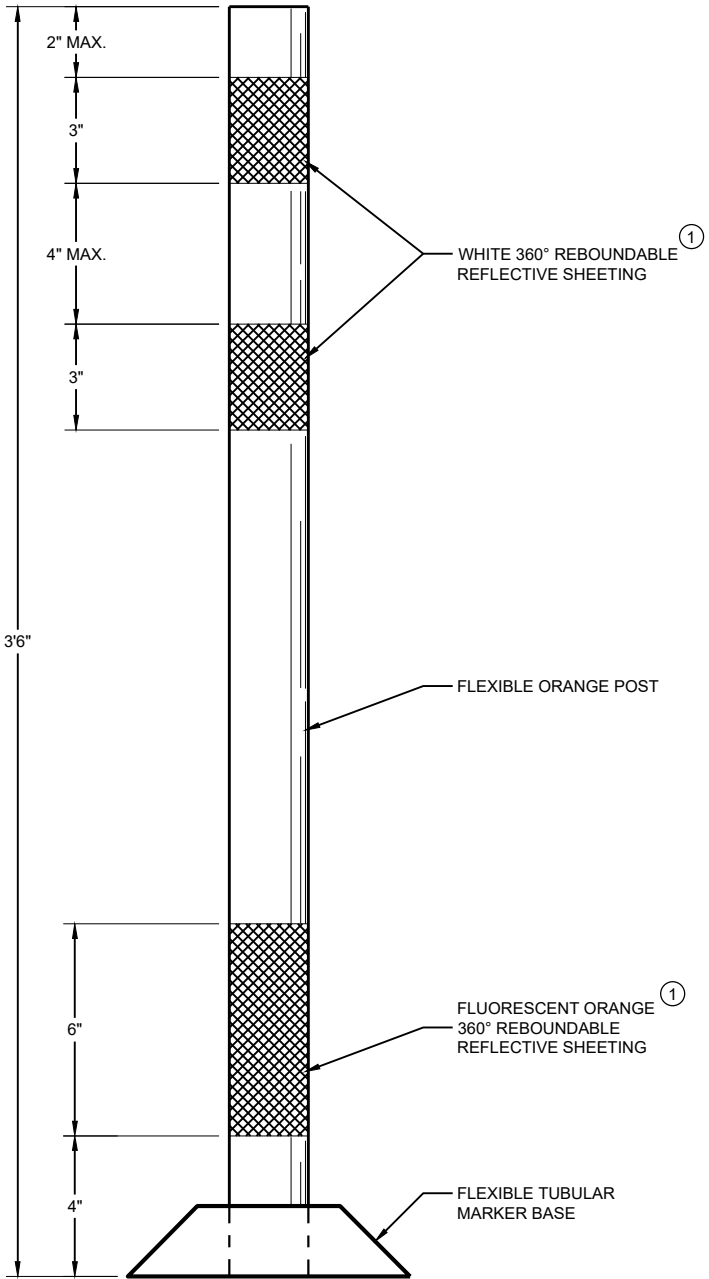
**CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"**

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**





FLEXIBLE TUBULAR
MARKER POST
PERMANENT CROSSOVER



FLEXIBLE TUBULAR
MARKER POST
WORK ZONE

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST FOUNDATION WHEN SECURED TO THE PAVEMENT.

THE ASPHALTIC ADHESIVE OR BUTYL PAD FURNISHED SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS, UNLESS DIRECTED BY THE ENGINEER TO USE BOLTS.

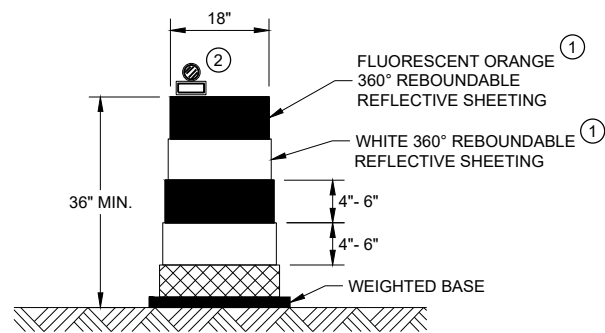
① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

CHANNELIZING DEVICES
FLEXIBLE TUBULAR
MARKER POST

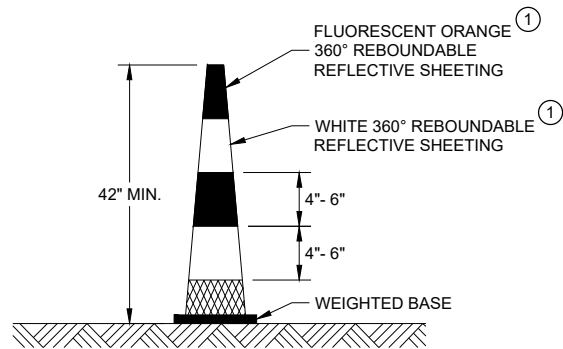
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

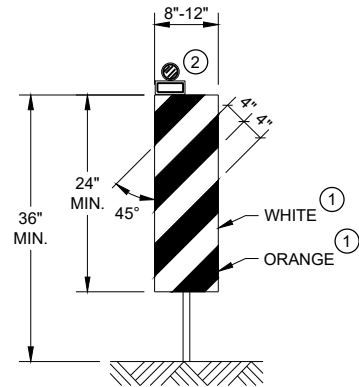


DRUM



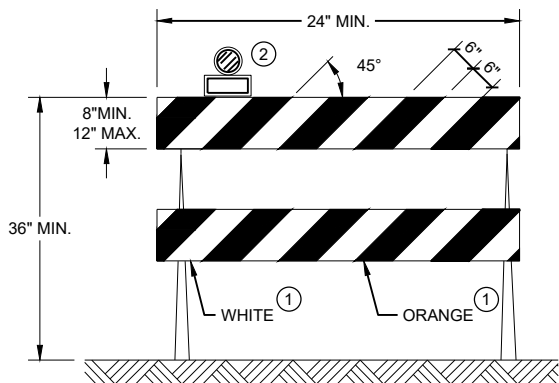
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS



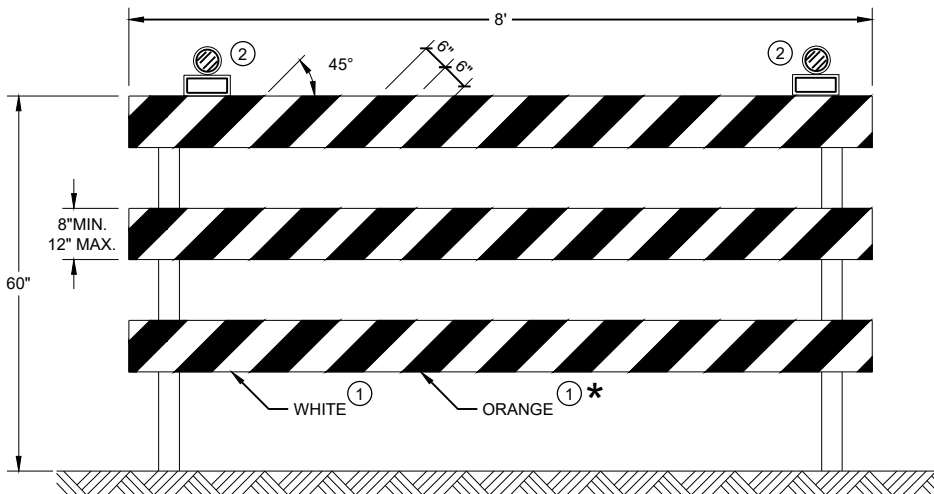
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.






CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

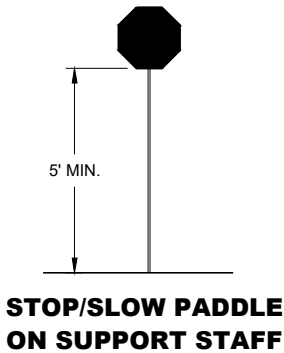
③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

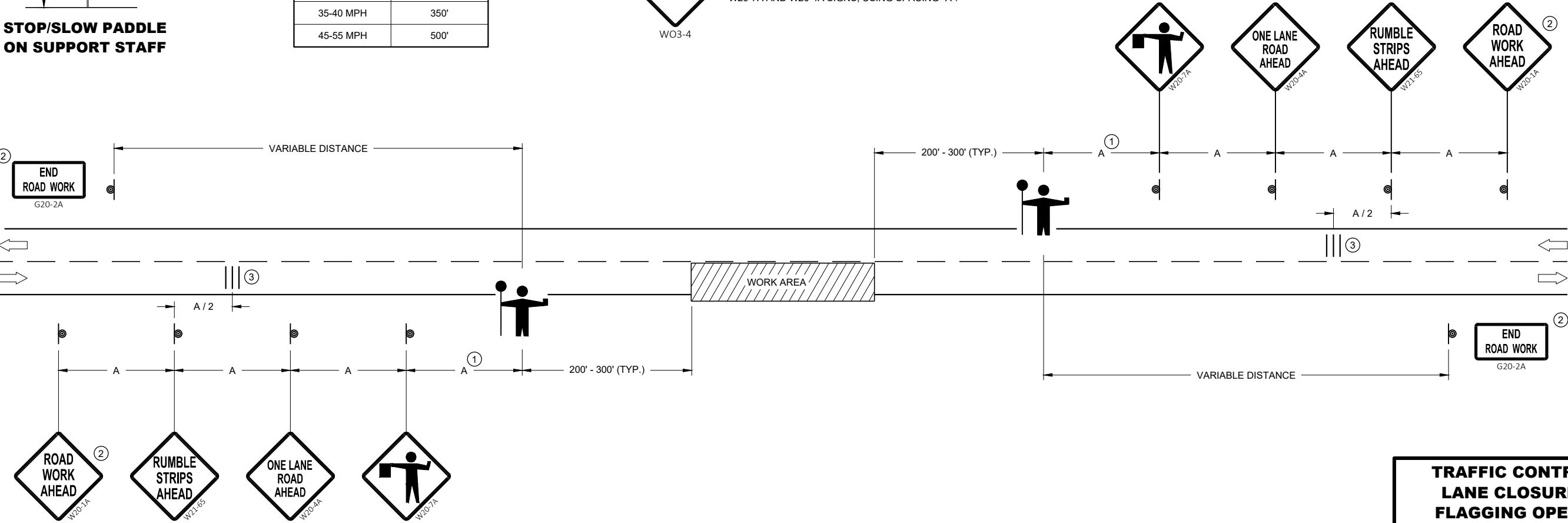


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

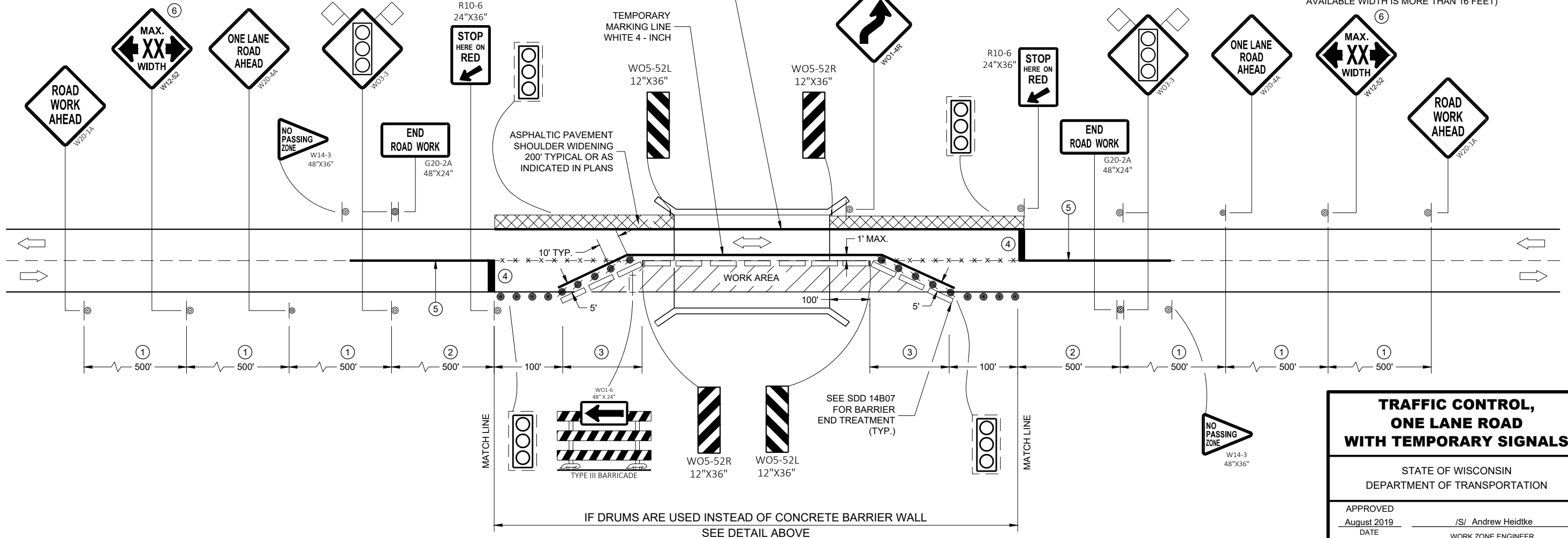
APPROVED
May 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

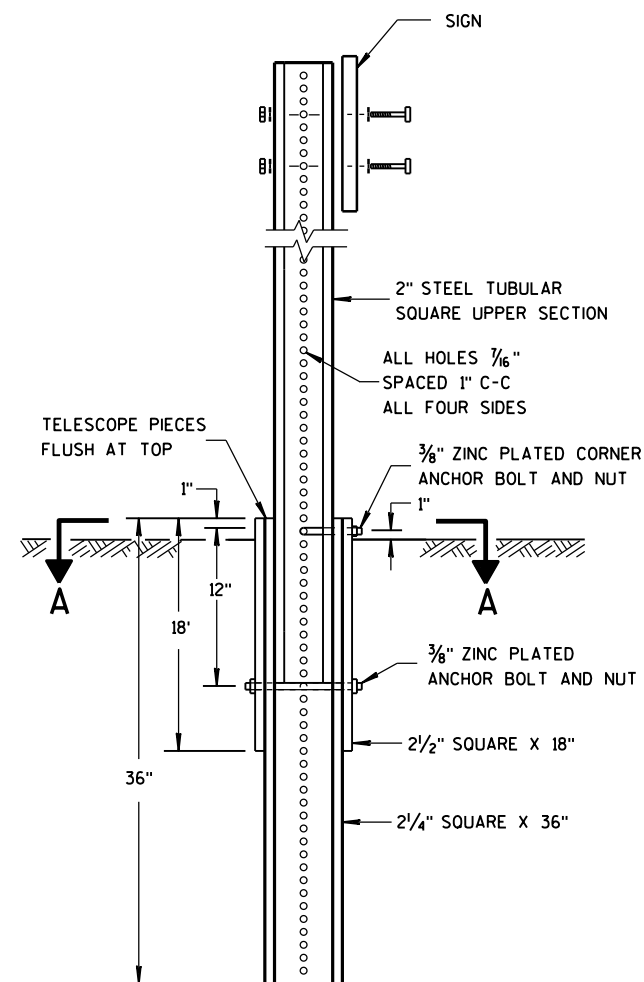
- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLAGS, 16" X 16" MIN. (ORANGE)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- ASPHALTIC PAVEMENT WIDENING
- CONCRETE BARRIER TEMPORARY PRECAST
- TEMPORARY SIGNAL. SEE SDD 09G02 FOR EXACT PLACEMENT

WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)



GENERAL NOTES

- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..
- THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.
- ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.
- "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.
- PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.
- 500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.
 - USE 300 FOOT SPACING IF THE PRE - CONSTRUCTION REGULATORY SPEED IS 35 MPH OR LESS.
 - DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.
 - TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18 - INCH.
 - 700 FOOT TEMPORARY MARKING LINE, DOUBLE YELLOW 4 - INCH . WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
 - SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.

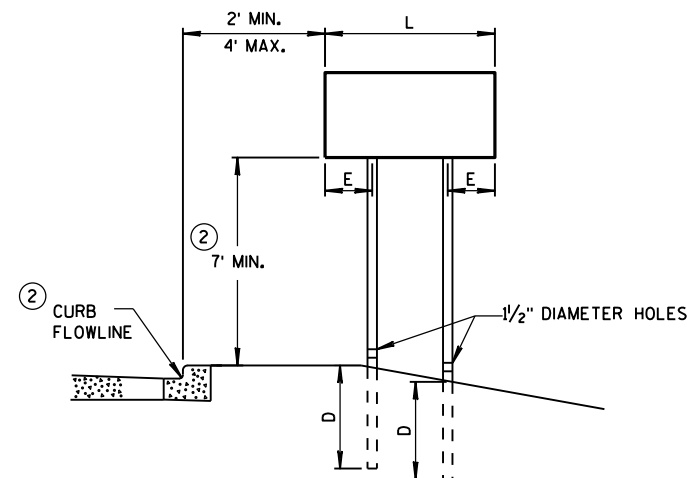
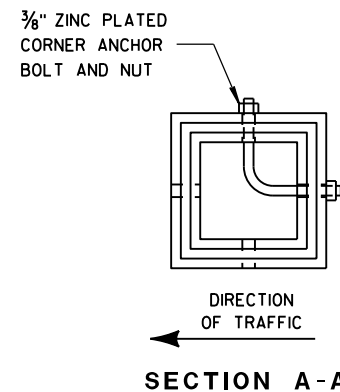


DETAIL OF TUBULAR
STEEL SIGN POST

TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

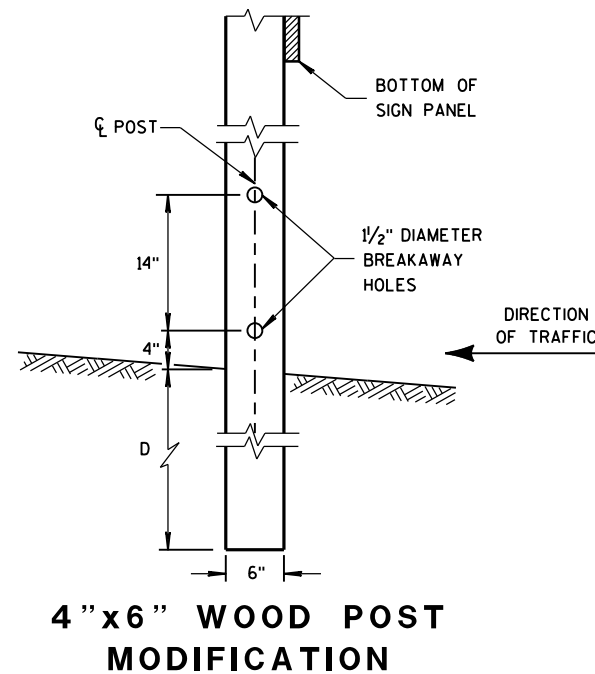
SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL
BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).
SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED
ON TUBULAR STEEL POSTS.



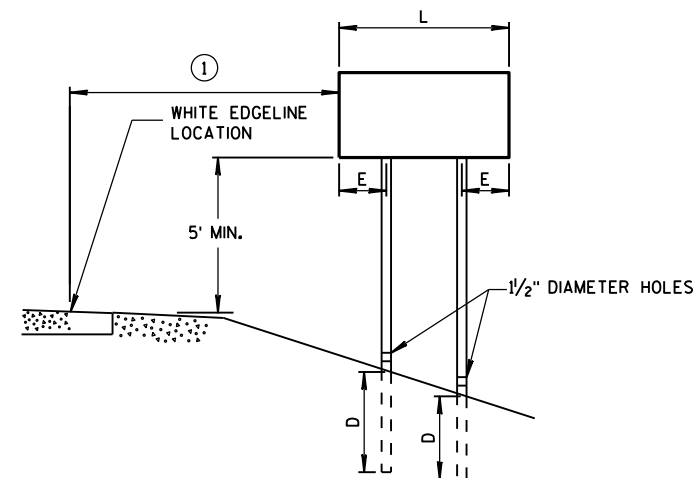
URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH	
AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



4 "x6 " WOOD POST
MODIFICATION



RURAL AREA

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

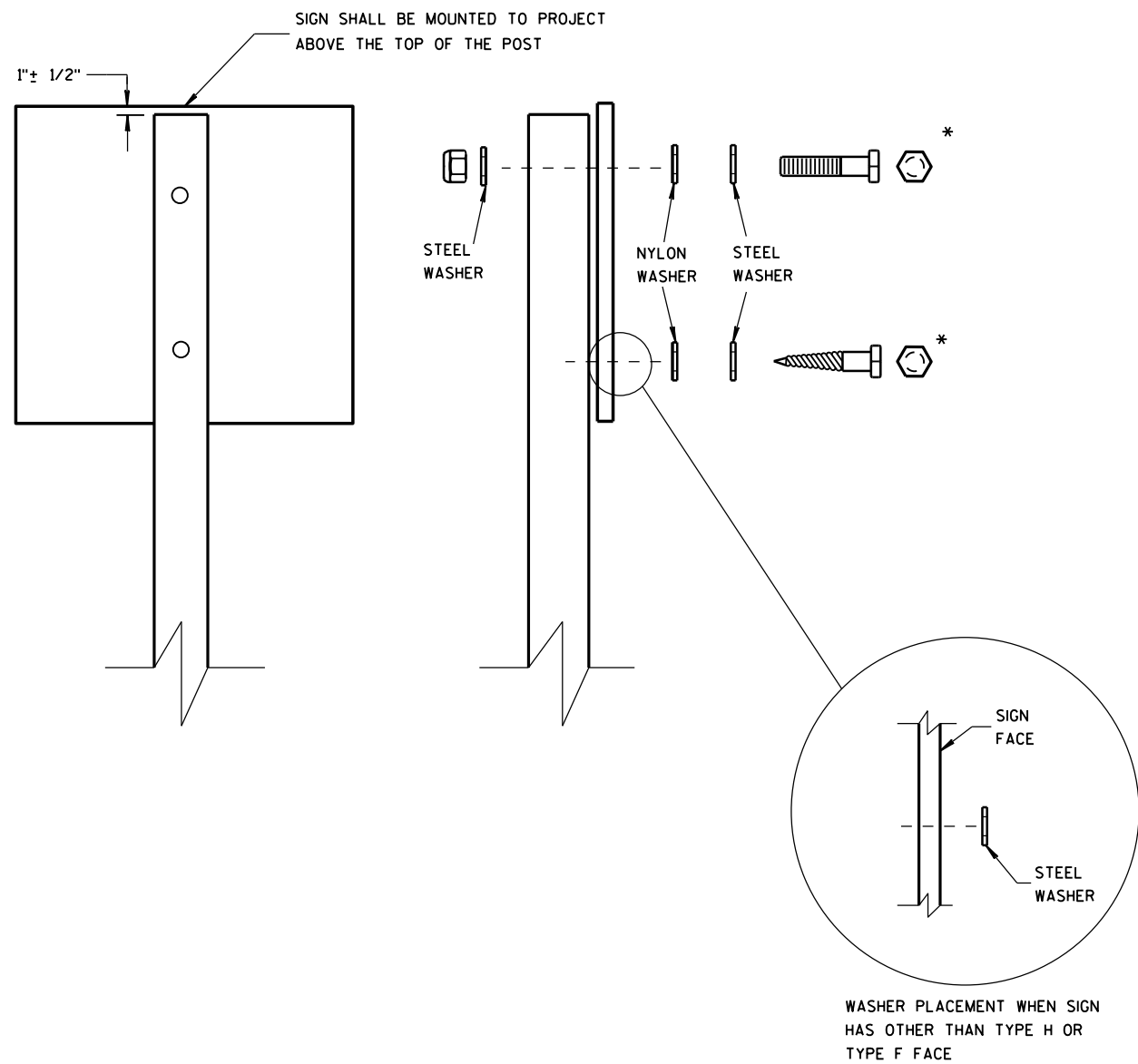
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL
SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

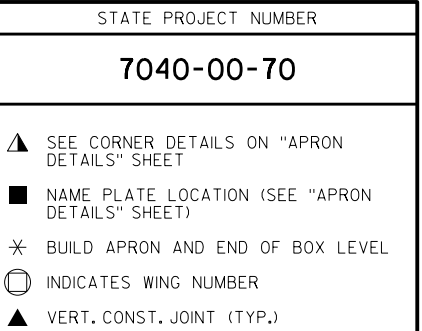
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" x 3"
 - MACHINE BOLTS - 5/16" x 6-1/2" OR 7" LENGTH W/ NUTS

- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" x 3-1/4" LENGTH W/ NUTS
 - RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

- WASHERS (ALL POSTS) -
- 1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL
 - 1-1/4" O.D. x 3/8" I.D. x .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heldtke WORK ZONE ENGINEER
FHWA	




1. LAYOUT
2. BOX DETAILS
3. APRONS
4. APRON DETAILS
5. SUBSURFACE EXPLORATION

THE CONTRACTOR MAY ELECT TO SUBSTITUTE #1OR #2 CONCRETE COARSE AGGREGATE, SELECT CRUSHED MATERIAL OR OTHER GRANULAR MATERIAL AS APPROVED BY THE FIELD ENGINEER, IN LIEU OF THE BREAKER RUN, TO BE UTILIZED AS A CONSTRUCTION PLATFORM FOR THE BOX. THE CONTRACTOR IS RESPONSIBLE FOR BASE STABILITY WITH ANY SUBSTITUTED MATERIAL.

Q₂ = 115 C.F.S.
VEL.₂ = 3.7 F.P.S.
HW.₂ = EL. 1261.56

STH 98
ADT = 3,200 (2041)
R.D.S. = 60 M.P.H.

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
203.0200	REMOVING OLD STRUCTURE STA. 497+82.7±	LS	1
206.2000	EXCAVATION FOR STRUCTURES CULVERTS C-10-17	LS	1
210.2500	BACKFILL STRUCTURE TYPE B	TON	790
311.0115	BREAKER RUN	CY	80
504.0100	CONCRETE MASONRY CULVERTS	CY	148
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	25,560
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	640
511.1200	TEMPORARY SHORING C-10-17	SF	90
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	24
606.0300	RIPRAP HEAVY	CY	39
645.0105	GEOTEXTILE TYPE C	SY	253
645.0120	GEOTEXTILE TYPE HR	SY	99
	NON-BID ITEMS		
	FILLER	SIZE	¾"

NO.		DATE		REVISION		BY	
		BUREAU OF STRUCTURES					
ACCEPTED		<i>William C. Dreher</i> ^{LES}				2/21/20	
		CHIEF STRUCTURES DESIGN ENGINEER				DATE	
STRUCTURE C-10-17							
STH 98 OVER BRANCH ROCK CREEK							
COUNTY				CLARK		TOWN	
						SHERMAN	
DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS							
DESIGNED BY		DESIGNED CK'D.		DRAWN BY		PLANS CK'D.	
MWB				PJR		MWB	
						PJR	
LAYOUT						SHEET 1 OF 5	



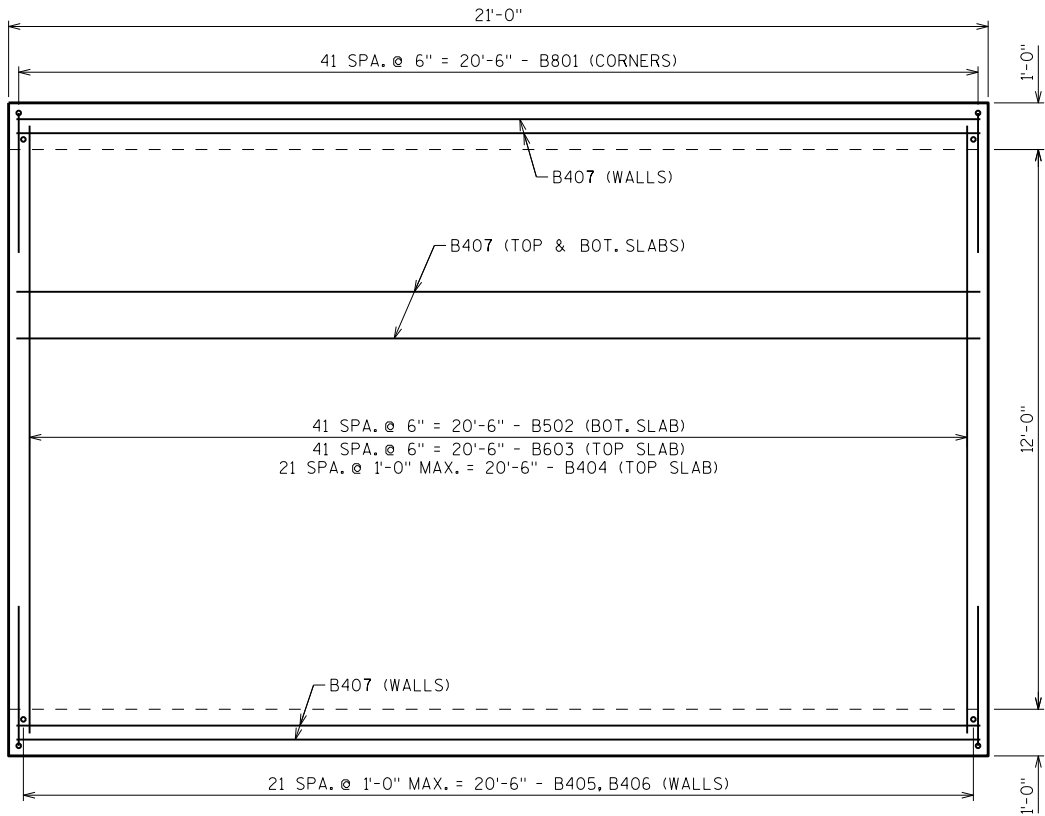
STATE PROJECT NUMBER

7040-00-70

BILL OF BARS

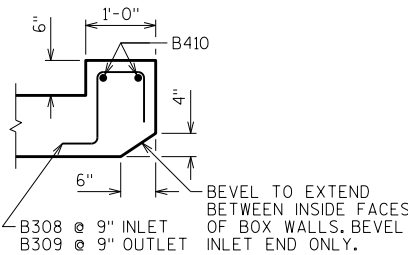
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
B801		672	7'-9"	X		CORNERS
B502		168	13'-6"			BOT. SLAB - HORIZ.
B603		168	13'-6"			TOP SLAB - HORIZ.
B404		88	10'-2"			TOP SLAB - HORIZ.
B405		176	2'-5"			EXT. WALLS - DOWELS
B406		176	4'-10"			EXT. WALLS - VERT.
B407		224	20'-6"			TOP & BOT. SLAB & WALLS - LONGIT.
B308		19	3'-3"	X		INLET HEADER - VERT.
B309		19	3'-7"	X		OUTLET HEADER - VERT.
B410		4	13'-8"			HEADERS - HORIZ.
B511		124	4'-0"			VERT. & APRON CONST. JOINTS



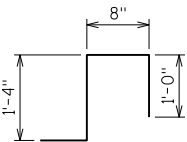
PLAN

SINGLE PANEL SHOWN, OTHER PANELS SIMILAR
HEADERS NOT SHOWN FOR CLARITY

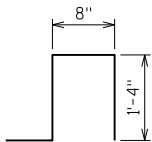


SECTION THRU HEADER

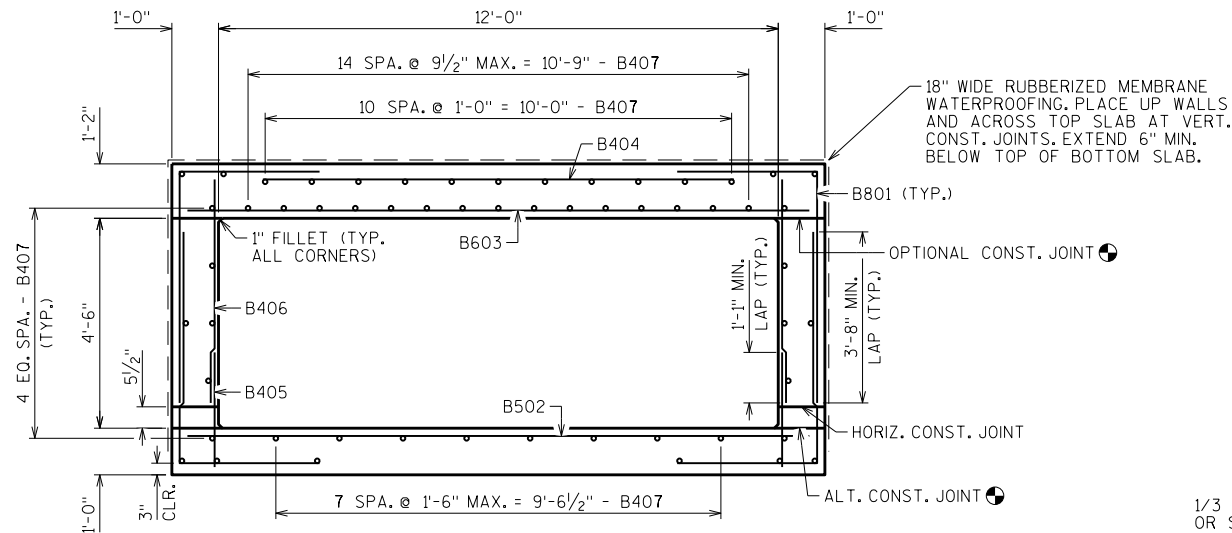
B801



B308

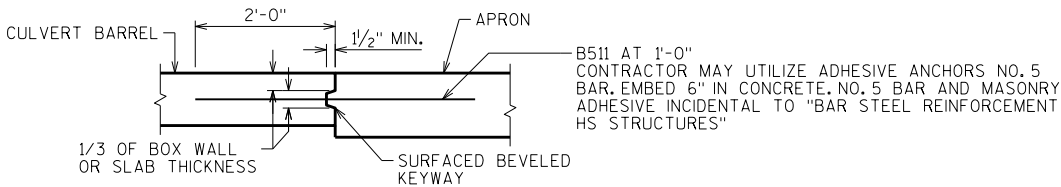


B309



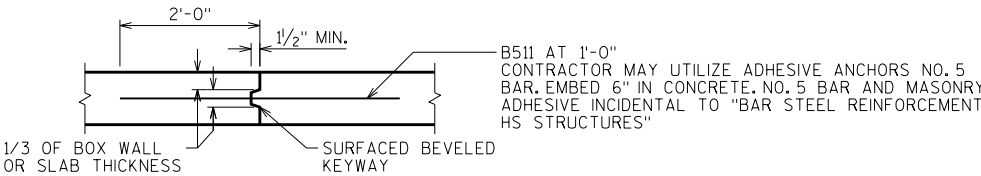
SECTION THRU BOX

ALL LONGITUDINAL BARS NOT LABELED ARE B407 AS SHOWN



APRON CONSTRUCTION JOINT

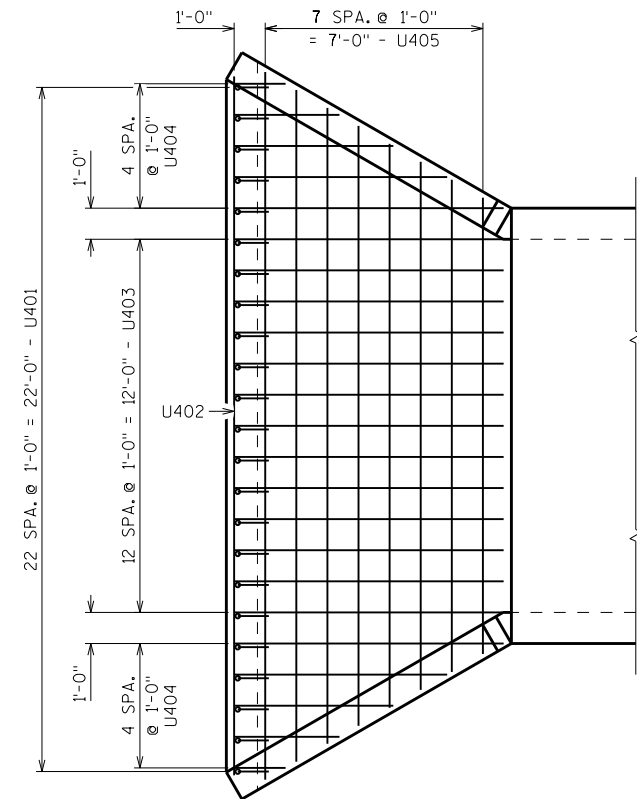
2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY
BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.



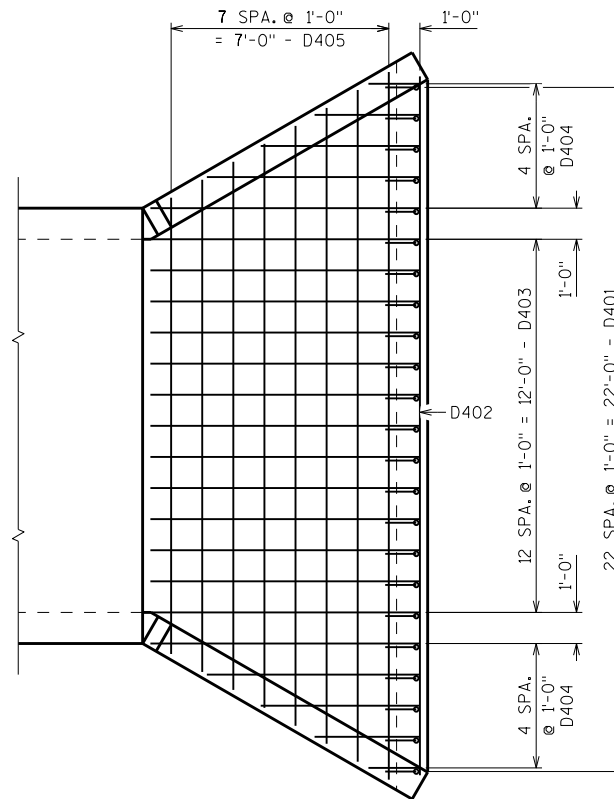
VERTICAL CONSTRUCTION JOINT

2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY
BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.

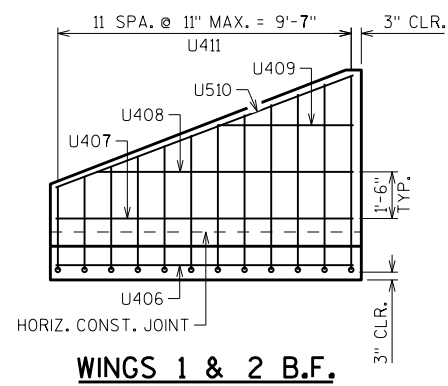
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-10-17			
DRAWN BY		MWB	PLANS CK'D. PJR
BOX DETAILS		SHEET 2	



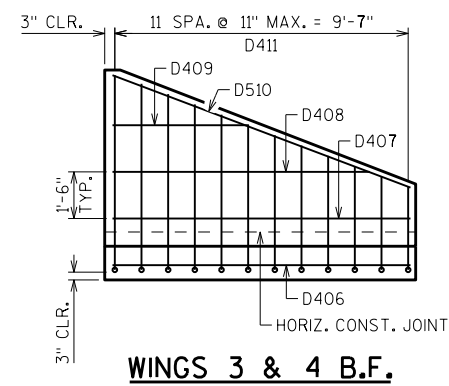
INLET PLAN



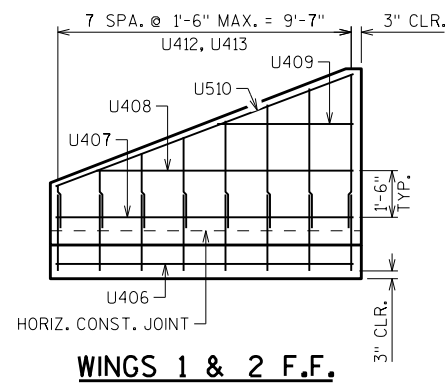
OUTLET PLAN



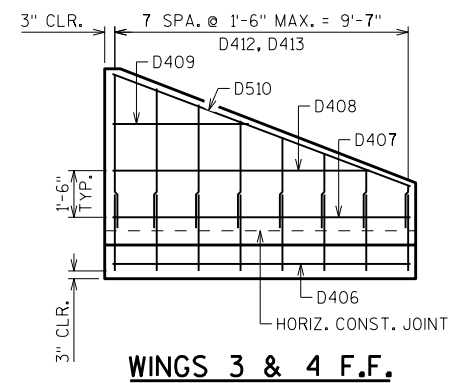
WINGS 1 & 2 B.F.



WINGS 3 & 4 B.F.



WINGS 1 & 2 F.F.

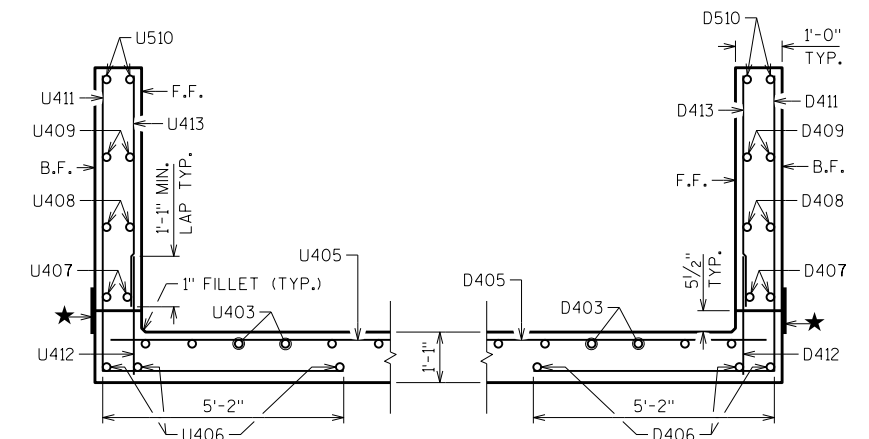


WINGS 3 & 4 F.F.

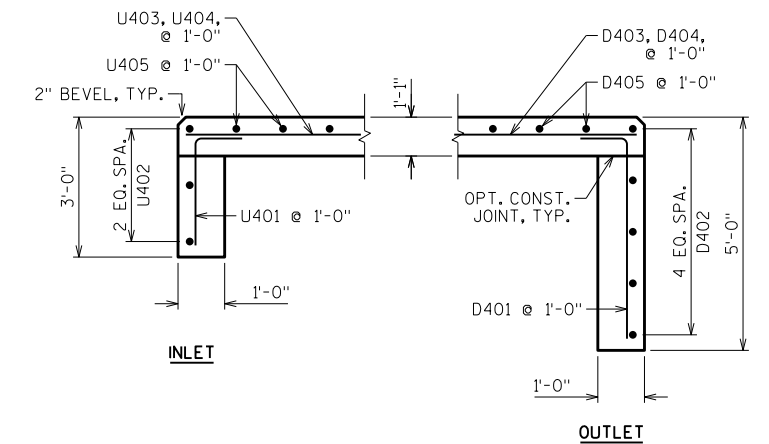
★ 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING ALONG HORIZ. CONST. JOINT IN WING.

STATE PROJECT NUMBER

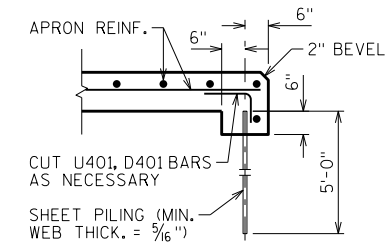
7040-00-70



SECTION THRU WINGS



CUT-OFF WALLS



ALTERNATE CUT-OFF WALLS

THE ABOVE ALTERNATIVE MAY BE USED IN LIEU OF CAST-IN-PLACE CONCRETE CUT-OFF WALLS. PAYMENT WILL BE BASED ON THE CONCRETE CUT-OFF WALLS.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-10-17			
DRAWN BY		MWB	PLANS CK'D. PJR
APRONS		SHEET 3	

SCALE = 3:00

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

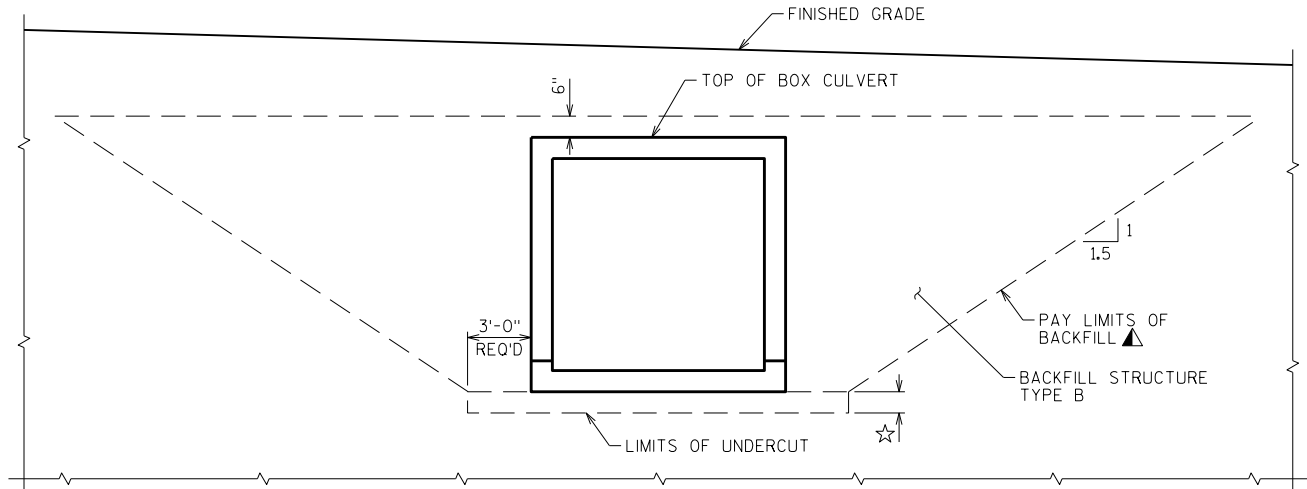
BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
U401		23	3'-6"	X		INLET APRON CUTOFF - VERT.
U402		3	22'-6"			INLET APRON CUTOFF - HORIZ.
U403		13	8'-10"			INLET APRON - LONGIT.
U404		10	5'-3"		▲	INLET APRON - LONGIT.
U405		8	18'-8"		▲	INLET APRON - HORIZ.
U406		6	9'-7"			WINGS 1 & 2 - HORIZ. - BOT.
U407	X	4	9'-7"			WINGS 1 & 2 - HORIZ.
U408	X	4	8'-3"			WINGS 1 & 2 - HORIZ.
U409	X	4	4'-5"			WINGS 1 & 2 - HORIZ.
U510	X	4	10'-3"			WINGS 1 & 2 - HORIZ. - TOP
U411	X	24	9'-8"	X	▲	WINGS 1 & 2 - VERT. - B.F.
U412	X	16	2'-5"			WINGS 1 & 2 - DOWELS - F.F.
U413	X	16	3'-3"		▲	WINGS 1 & 2 - VERT. - F.F.
D401		23	5'-6"	X		OUTLET APRON CUTOFF - VERT.
D402		5	22'-6"			OUTLET APRON CUTOFF - HORIZ.
D403		13	8'-10"			OUTLET APRON - LONGIT.
D404		10	5'-3"		▲	OUTLET APRON - LONGIT.
D405		8	18'-8"		▲	OUTLET APRON - HORIZ.
D406		6	9'-7"			WINGS 3 & 4 - HORIZ. - BOT.
D407	X	4	9'-7"			WINGS 3 & 4 - HORIZ.
D408	X	4	8'-3"			WINGS 3 & 4 - HORIZ.
D409	X	4	4'-5"			WINGS 3 & 4 - HORIZ.
D510	X	4	10'-3"			WINGS 3 & 4 - HORIZ. - TOP
D411	X	24	9'-8"	X	▲	WINGS 3 & 4 - VERT. - B.F.
D412	X	16	2'-5"			WINGS 3 & 4 - DOWELS - F.F.
D413	X	16	3'-3"		▲	WINGS 3 & 4 - VERT. - F.F.

▲ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

BAR SERIES TABLE

BAR MARK	NO. REQ'D.	LENGTH
U404	2 SERIES OF 5	1'-9" TO 8'-8"
U405	1 SERIES OF 8	14'-8" TO 22'-9"
U411	2 SERIES OF 12	7'-10" TO 11'-5"
U413	2 SERIES OF 8	1'-5" TO 5'-0"
D404	2 SERIES OF 5	1'-9" TO 8'-8"
D405	1 SERIES OF 8	14'-8" TO 22'-9"
D411	2 SERIES OF 12	7'-10" TO 11'-5"
D413	2 SERIES OF 8	1'-5" TO 5'-0"

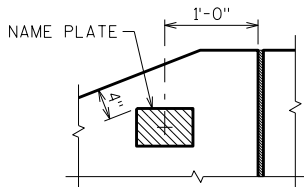
BUNDLE AND TAG EACH SERIES SEPARATELY.



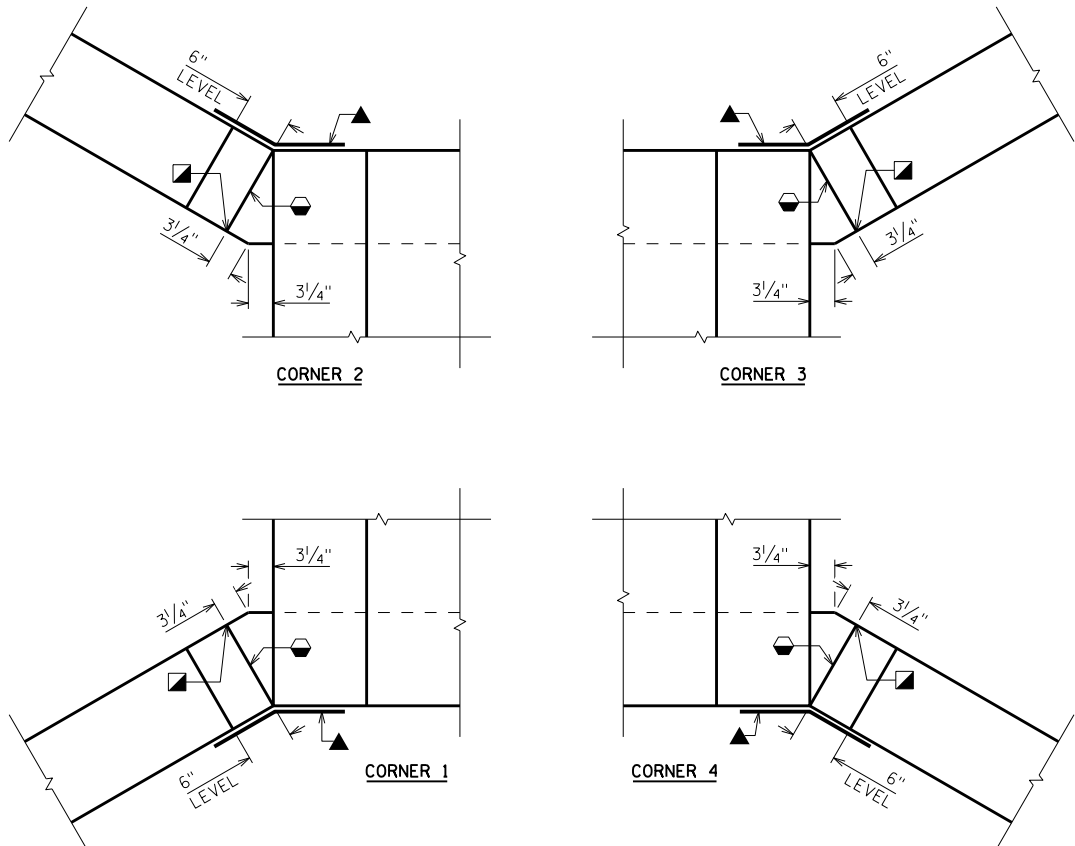
TYPICAL SECTION THRU BOX CULVERT

▲ BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

☆ UNDER CUT 1'-0". EXCAVATION FOR UNDER CUT TO BE INCLUDED IN EXCAVATION FOR STRUCTURES. PLACE "GEOTEXTILE TYPE C" AND BACKFILL WITH "BREAKER RUN".



NAME PLATE LOCATION WING 4



CORNER DETAILS

- 3/4" FILLER TO EXTEND FROM HORIZ. CONST. JOINT TO TOP OF WING.
- 1" BEVEL TYP.
- 18" WIDE RUBBERIZE MEMBRANE WATERPROOFING, EXTEND FROM HORIZ. CONST. JOINT TO TOP OF WING.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-10-17			
DRAWN BY		MWB	PLANS CK'D. PJR
APRON DETAILS		SHEET 4	

SCALE = 1:00

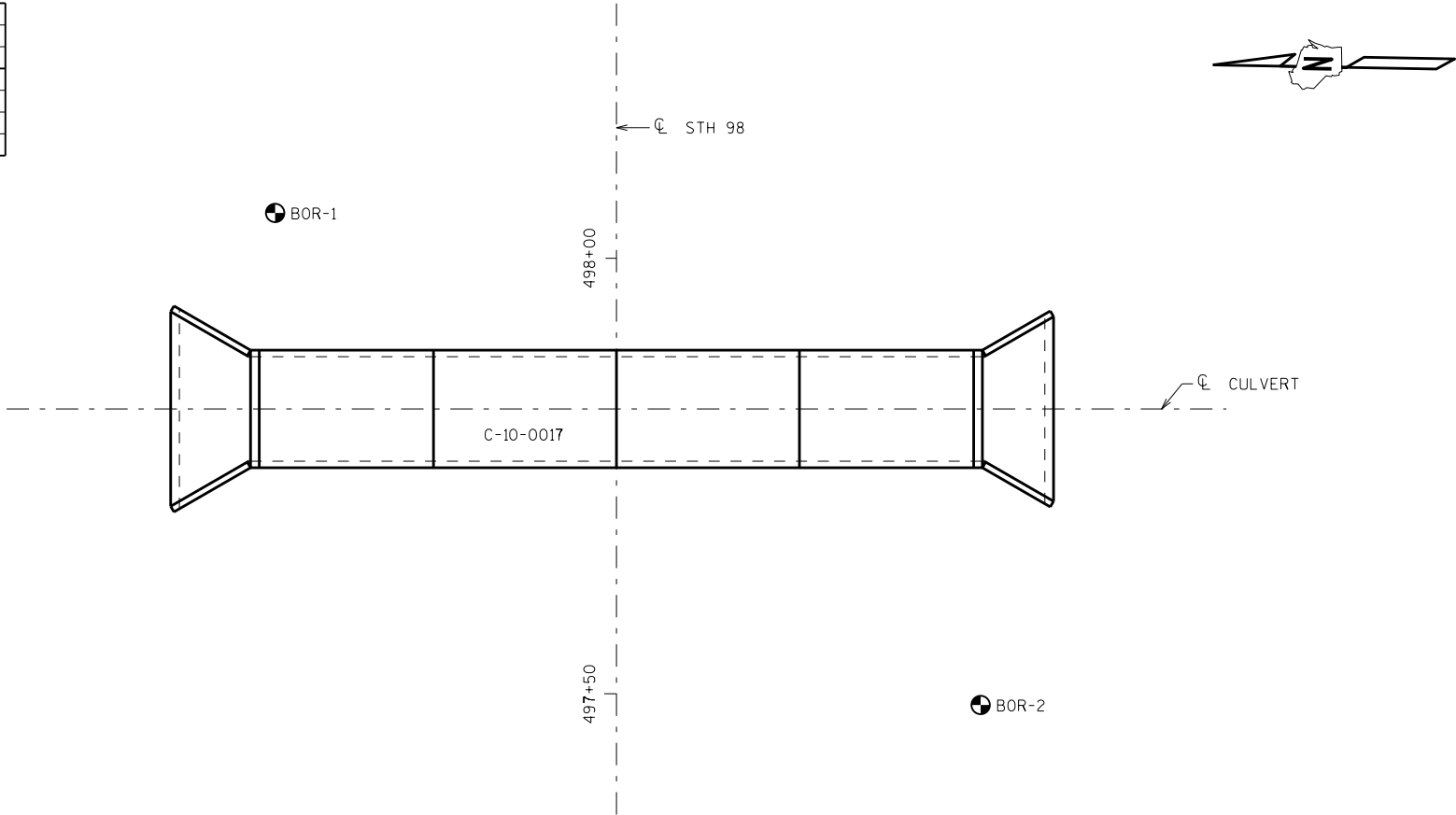
BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	5/15/2019	416412	732130
2	5/16/2019	416333	732075

BORINGS COMPLETED BY: WISDOT

REPORT COMPLETED BY: WISDOT

ALL COORDINATES REFERENCED TO WCCS NAD 83(91) CLARK COUNTY

COORDINATES COLLECTED USING NON-SURVEY GRADE EQUIPMENT



STATE PROJECT NUMBER

7040-00-70

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/META

LEGEND OF BORING

(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

▽ AT TIME OF DRILLING

▽ END OF DRILLING

▽ AFTER DRILLING

ABBREVIATIONS

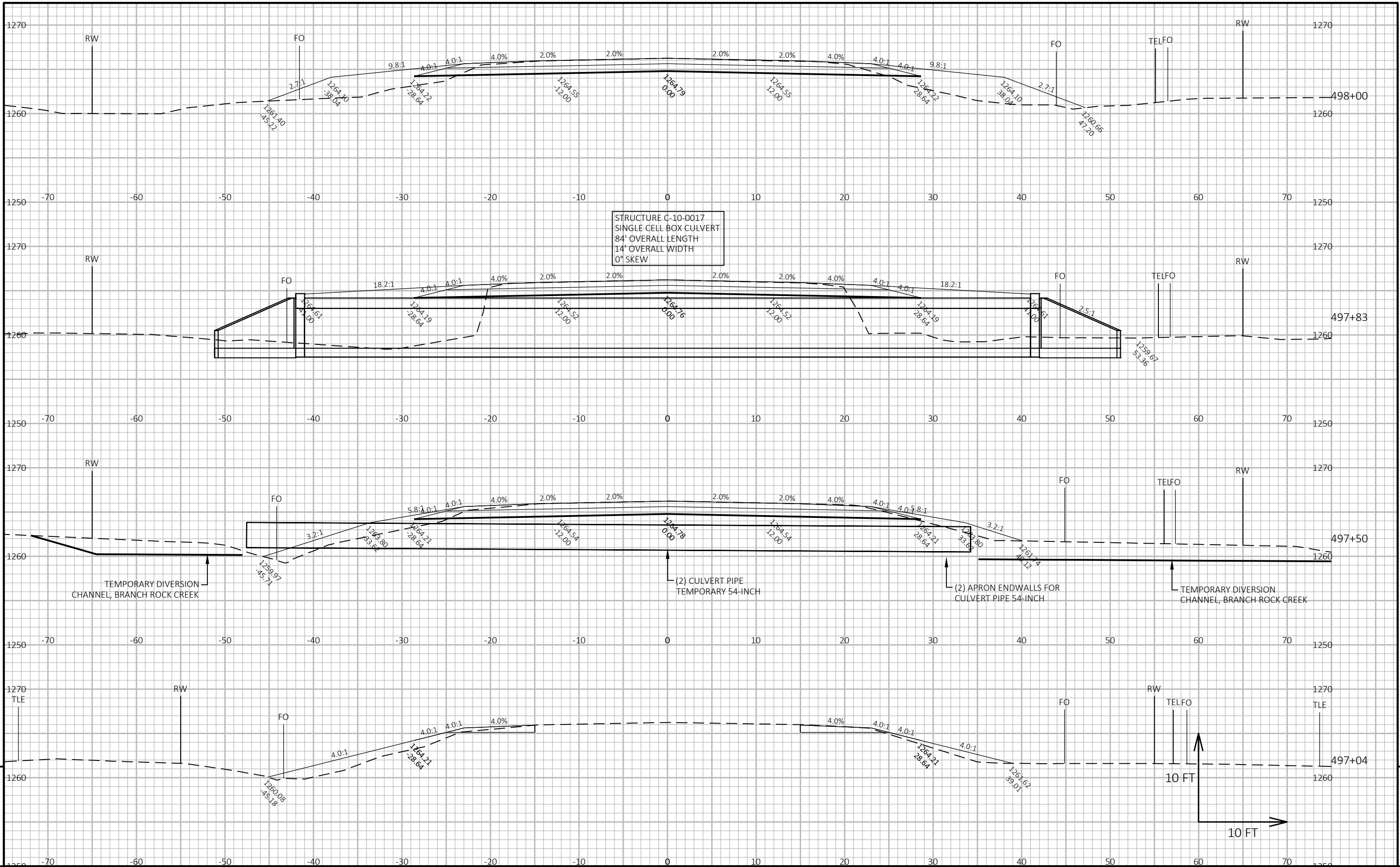
F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-10-17			
DRAWN BY TLP/MWB		PLANS CK'D. PJR	
SUBSURFACE EXPLORATION		SHEET 5	

SCALE = 10.00



PROJECT NO: 7040-00-70

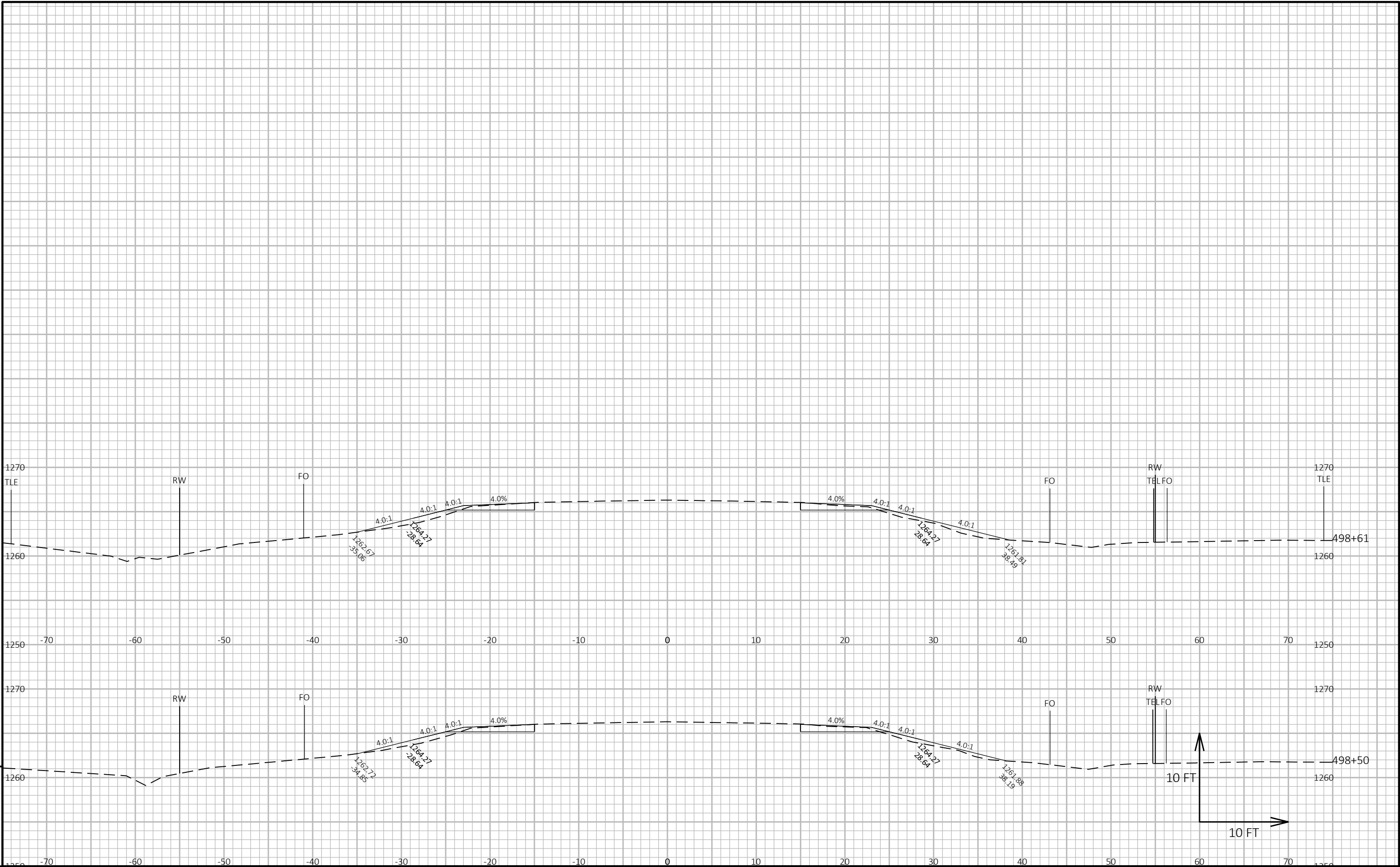
HWY: STH 98

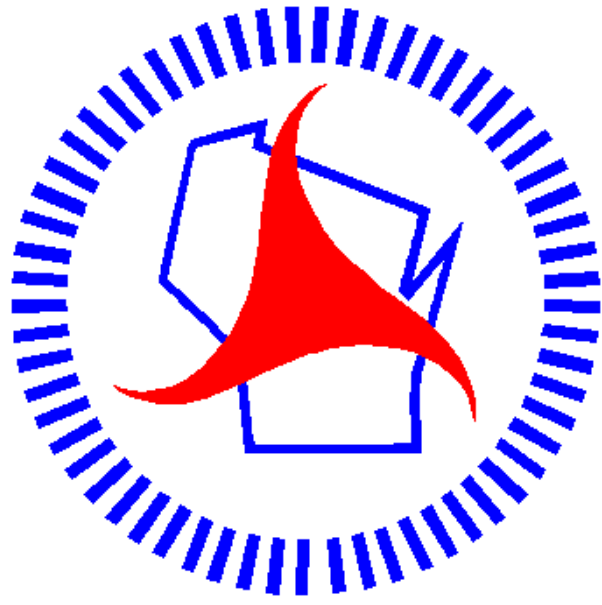
COUNTY: CLARK

CROSS SECTIONS: STH 98

SHEET

E





Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>