

EAU MARCH 2020

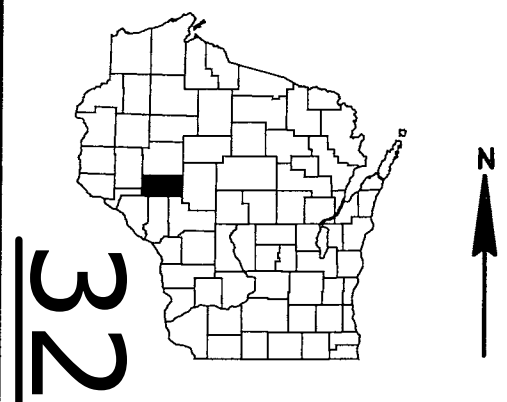
ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plans
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 140

PROJECT ID: 7080-01-72

COUNTY: EAU CLAIRE



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

EAU CLAIRE - FALL CREEK

ELCO RD TO OAK KNOLL RD

USH 12

EAU CLAIRE

STATE PROJECT NUMBER
7080-01-72

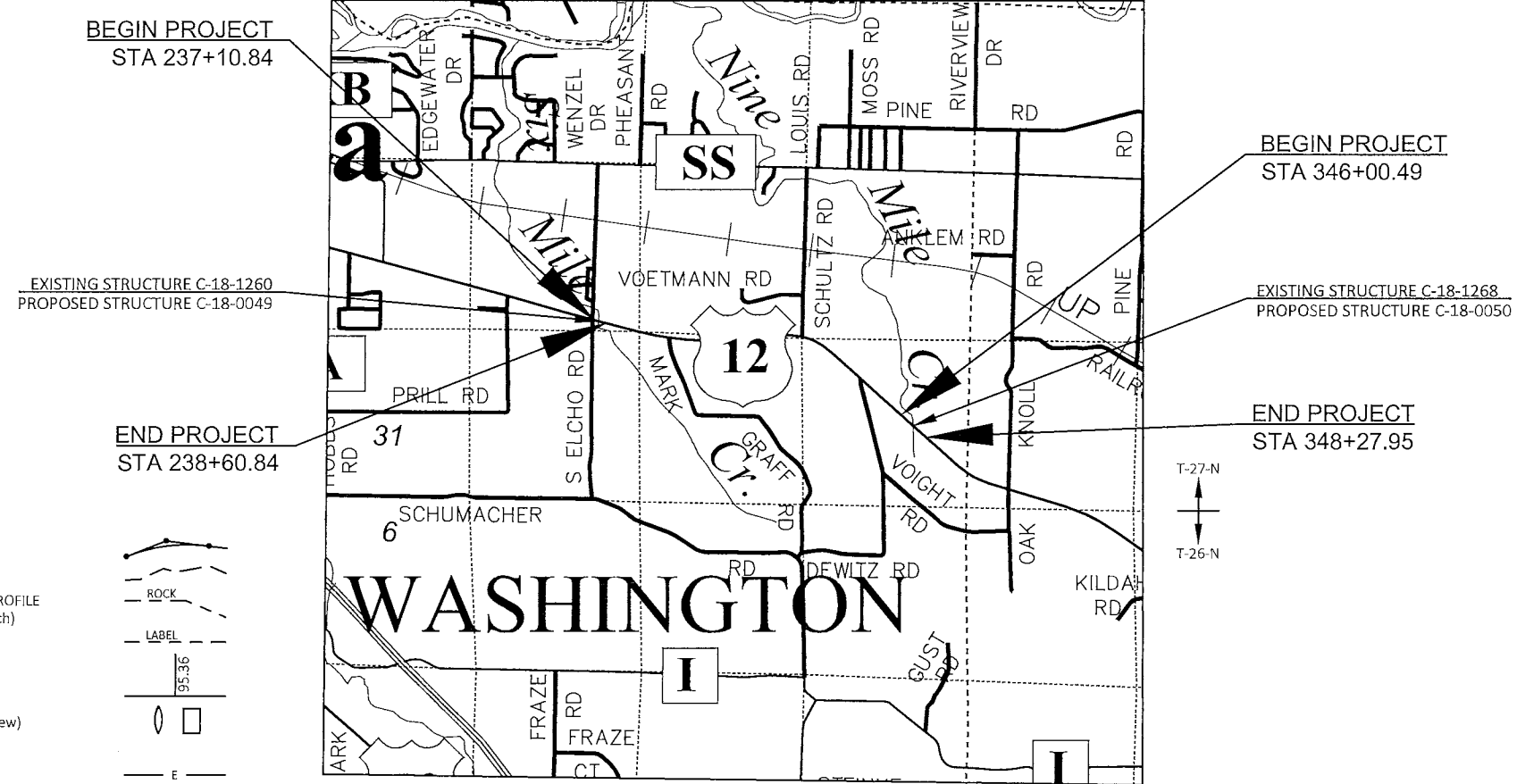
STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7080-01-72	WISC 2020089	1

DESIGN DESIGNATION

	C-18-0049	C-18-0050
A.A.D.T. 2020	= 9700	5600
A.A.D.T. 2040	= 13300	7700
D.H.V.	= 1357 (10.2%)	770 (10.0%)
D.D.	= 59/41	60/40
T.	= 11.9%	11.9%
DESIGN SPEED	= 60 MPH	60 MPH
ESALS	= 1,900,000	1,100,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE



LAYOUT
SCALE 0 1 MI
TOTAL NET LENGTH OF CENTERLINE = 0.072 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	WISDOT
Surveyor	MATTHEW PAYNE
Designer	DAVID KOEPP
Project Manager	JENNIFER OLDENBURG
Regional Examiner	JAMES KOENIG
Regional Supervisor	

APPROVED FOR THE DEPARTMENT
DATE: 1/16/2020 [Signature]
(Signature)

E

LIST OF STANDARD ABBREVIATIONS

ABUT	ABUTMENT	EXIST	EXISTING	R/W	RIGHT OF WAY
AGG	AGGREGATE	FERT	FERTILIZE	RDWY	ROADWAY
AH	AHEAD	FO	FIBER OPTIC	SL	SLOPE
APPROX	APPROXIMATE	FE	FIELD ENTRANCE	SQ	SQUARE
ASPH	ASPHALTIC	FIN	FINISHED	SF	SQUARE FOOT
AVG	AVERAGE	FT	FOOT	SY	SQUARE YARD
ADT	AVERAGE DAILY TRAFFIC	FL	FLOW LINE	STD	STANDARD
AZ	AZIMUTH	GA	GAUGE	SDD	STANDARD DETAIL DRAWING
BK	BACK	G	GRADE	STH	STATE TRUNK HIGHWAY
BEG	BEGIN	HORIZ	HORIZONTAL	STA	STATION
BM	BENCHMARK	HMA	HOT MIX ASPHALT	STRUCT	STRUCTURE
BR	BRIDGE	CWT	HUNDREDWEIGHT	SE	SUPERELEVATION
C/L	CENTERLINE	INL	INLET	SURF	SURFACE
Δ	CENTRAL ANGLE OR DELTA	INV	INVERT	T	TANGENT LENGTH
CE	COMMERCIAL ENTRANCE	K	RATE OF VERTICAL CURVATURE	TEL	TELEPHONE
CONC	CONCRETE	LT	LEFT	TLE	TEMPORARY LIMITED EASEMENT
CONST	CONSTRUCTION	LHF	LEFT-HAND FORWARD	TN	TOWN
CMP	CORRUGATED METAL PIPE	L	LENGTH OF CURVE	T	TRUCKS (PERCENT OF)
CO	COUNTY	LIN	LINEAR	TYP	TYPICAL
CTH	COUNTY TRUNK HIGHWAY	LF	LINEAR FOOT	UNCL	UNCLASSIFIED
X-SEC	CROSS SECTION	LS	LUMP SUM	UG	UNDERGROUND
CR	CRUSHED	MAX	MAXIMUM	VAR	VARIABLE
CFS	CUBIC FEET/SECOND	MI	MILE	V	VELOCITY OR DESIGN SPEED
CY, CU YD	CUBIC YARD	MIN	MINIMUM	VC	VERTICAL CURVE
CULV	CULVERT	MISC	MISCELLANEOUS	YD	YARD
CPCP	CULVERT PIPE CORRUGATED POLYETHYLENE	PAV'T	PAVEMENT		
CPRP	CULVERT PIPE REINFORCED CONCRETE	PLE	PERMANENT LIMITED EASEMENT		
DOT	DEPARTMENT OF TRANSPORTATION	PC	POINT OF CURVATURE		
DHV	DESIGN HOUR VOLUME	PI	POINT OF INTERSECTION		
DIA	DIAMETER	PT	POINT OF TANGENCY		
D	DIRECTIONAL DISTRIBUTION	PVC	POINT OF VERTICAL CURVATURE		
DISCH, DIS	DISCHARGE	PVI	POINT OF VERTICAL INTERSECTION		
DWY	DRIVEWAY	PVT	POINT OF VERTICAL TANGENCY		
EA	EACH	LB	POUND		
ELECT.	ELECTRIC	PE	PRIVATE ENTRANCE		
ELEV, EL	ELEVATION	PROJ	PROJECT		
EMB	EMBANKMENT	RAD	RADIUS		
ENT	ENTRANCE	R	RANGE		
ESALS	EQUIVALENT SINGLE AXLE LOADS	REQ'D	REQUIRED		
EXC	EXCAVATION	RT	RIGHT		
EBS	EXCAVATION BELOW SUBGRADE	RHF	RIGHT-HAND FORWARD		

GENERAL NOTES

ELEVATIONS SHOWN ON THE PLAN ARE REFERENCED TO USGS NAVD 88.

THE LOCATION OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

UTILITY REFERENCE LINES ON THE CROSS SECTIONS ARE FOR APPROXIMATE HORIZONTAL REFERENCE ONLY.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

EROSION CONTROL DEVICES AS SHOWN IN THE PLANS AREA T SUGGESTED LOCATIONS. THE ENGINEER MAY MODIFY LOCATIONS AS NEEDED. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THOSE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE SALVAGED TOPSOIL, FERTILIZED, SEEDED AND HAVE EROSION MAT INSTALLED AS DIRECTED BY THE ENGINEER.

WETLANDS HAVE BEEN DELINEATED WITHIN THE PROJECT LIMITS. DO NOT OPERATE OR STAGE EQUIPMENT IN WETLAND AREAS. THE ENGINEER SHALL CONTACT THE WISDOT ENVIRONMENTAL COORDINATOR 10 DAYS PRIOR TO THE START OF CONSTRUCTION OR BRIDGE REMOVAL TO UPDATED WETLAND DELINEATION.

THE ASPHALTIC SURFACE SHALL MEET THE REQUIREMENTS FOR 4 MT 58-34 S MIX, OR GREATER, WITH LAYER THICKNESSES FOLLOWING THE TABLE PROVIDED IN THE TYPICAL SECTIONS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

SILT FENCE IS TO BE PLACED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR IS RESPONSIBLE FOR RESHAPING AND FINISHING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY THEIR OPERATIONS OUTSIDE THE PLAN CONSTRUCTION LIMITS.

UTILITY CONTACTS

COMMUNICATIONS

ASTREA
 RUSSELL KENNY
 105 KENT ST
 P.O. BOX 190
 IRON MOUNTAIN, MI 49801-0190
 MOBILE PHONE: (906) 282-6434
 EMAIL: RUSSELL.KENNY@ASTREACONNECT.COM

AT&T WISCONSIN
 RICK PODOLAK
 304 SOUTH DEWEY ST, 4TH FLOOR
 EAU CLAIRE, WI 54701
 OFFICE PHONE: (715) 839-5565
 MOBILE PHONE: (715) 410-0656
 EMAIL: RP4514@ATT.COM

CENTURYLINK
 BRET CLARK
 311 S COURT ST
 SPARTA, WI 54656
 OFFICE PHONE: (608) 269-0819
 MOBILE PHONE: (608) 487-0637
 EMAIL: BRET.CLARK@CENTURYLINK.COM
 24-HOUR EMERGENCY REPAIR: 800-824-2877

CHARTER COMMUNICATIONS
 SHANE YODER
 1201 MCCANN DRIVE
 ALTOONA, WI 54720
 OFFICE PHONE: (715) 831-8940 EXT 51113
 MOBILE PHONE: (715) 370-7870
 EMAIL: SHANE.YODER@CHARTER.COM

ELECTRICITY

DAIRYLAND POWER COOPERATIVE
 ROB MALY
 3200 EAST AVE S
 P.O. BOX 817
 LA CROSSE, WI 54602-0817
 OFFICE PHONE: (608) 787-1427
 EMAIL: ROB.MALY@DAIRYLANDPOWER.COM

EAU CLAIRE ENERGY COOPERATIVE
 PAT BETHKE
 8214 USH 12
 P.O. BOX 368
 FALL CREEK, WI 54742-0368
 OFFICE PHONE: (715) 836-6479
 EMAIL: PBETHKE@ECEC.COM

GAS/PETROLEUM

XCEL ENERGY
 BRADY GARDOW
 1414 W HAMILTON AVE
 P.O. BOX 8
 EAU CLAIRE, WI 54702-0008
 OFFICE PHONE: (715) 737-1450
 EMAIL: BRADY.P.GARDOW@XCELENERGY.COM
 24-HOUR EMERGENCY: 800-895-2999

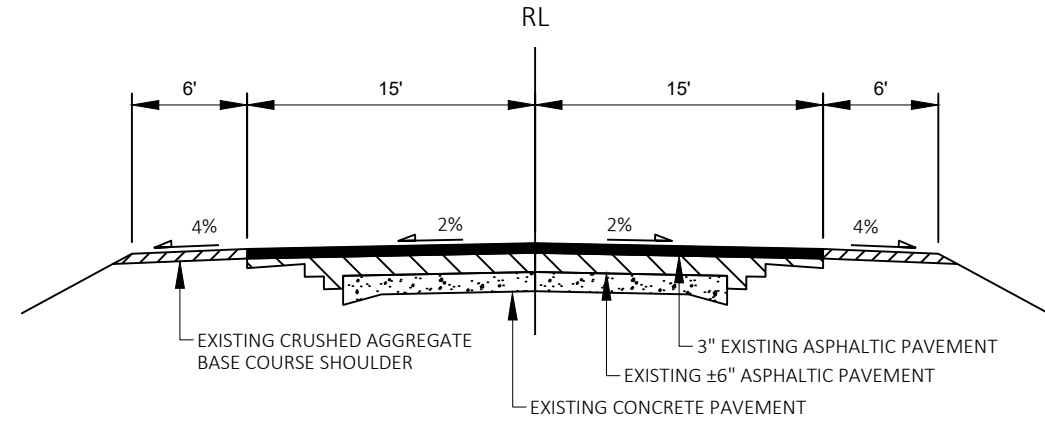


DNR LIAISON

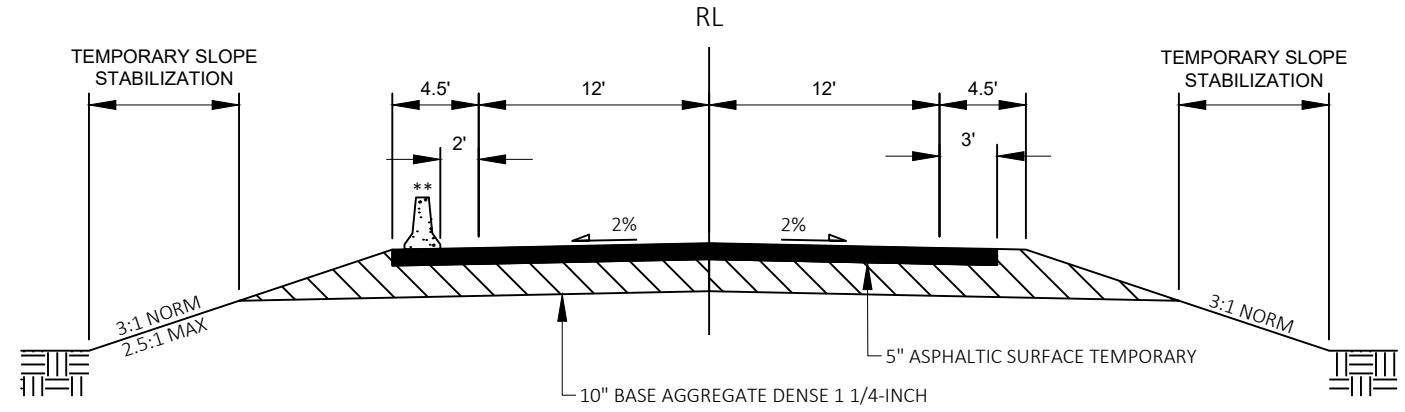
DEPARTMENT OF NATURAL RESOURCES
 LEAH NICOL
 1300 WEST CLAIREMONT AVENUE
 EAU CLAIRE, WI 54701
 PHONE: (715) 934-9014
 EMAIL: LEAH.NICOL@WISCONSIN.OV

ORDER OF DETAIL SHEETS

- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- SUPERELEVATIONS
- ALIGNMENT CONTROL
- EROSION CONTROL
- TRAFFIC CONTROL
- SIGNING & MARKING

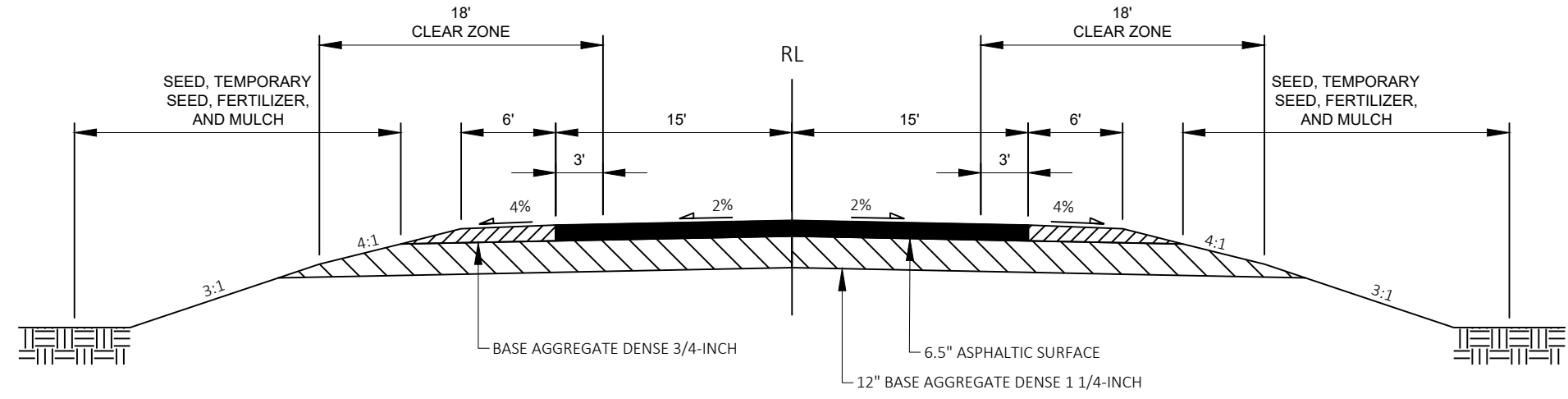


EXISTING TYPICAL SECTION USH 12
 STA 237+11 TO 238+61
 STA 346+00 TO 348+28

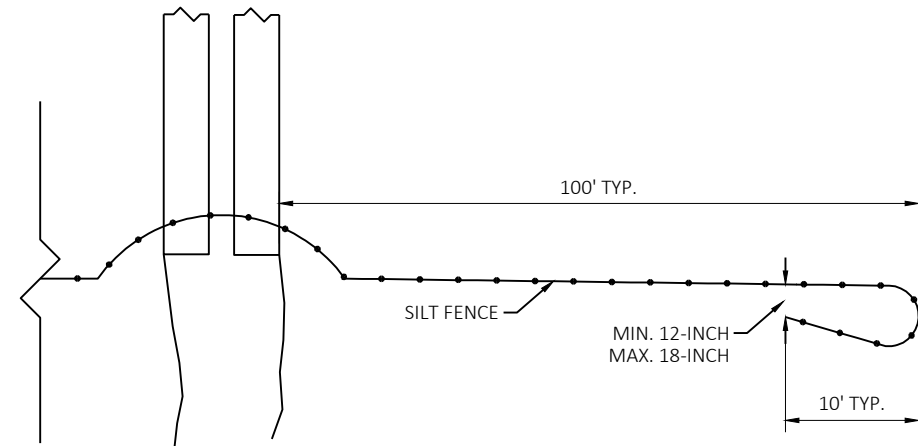


** CONCRETE BARRIER USED WHEN ADJACENT SLOPE IS GREATER THAN 3:1

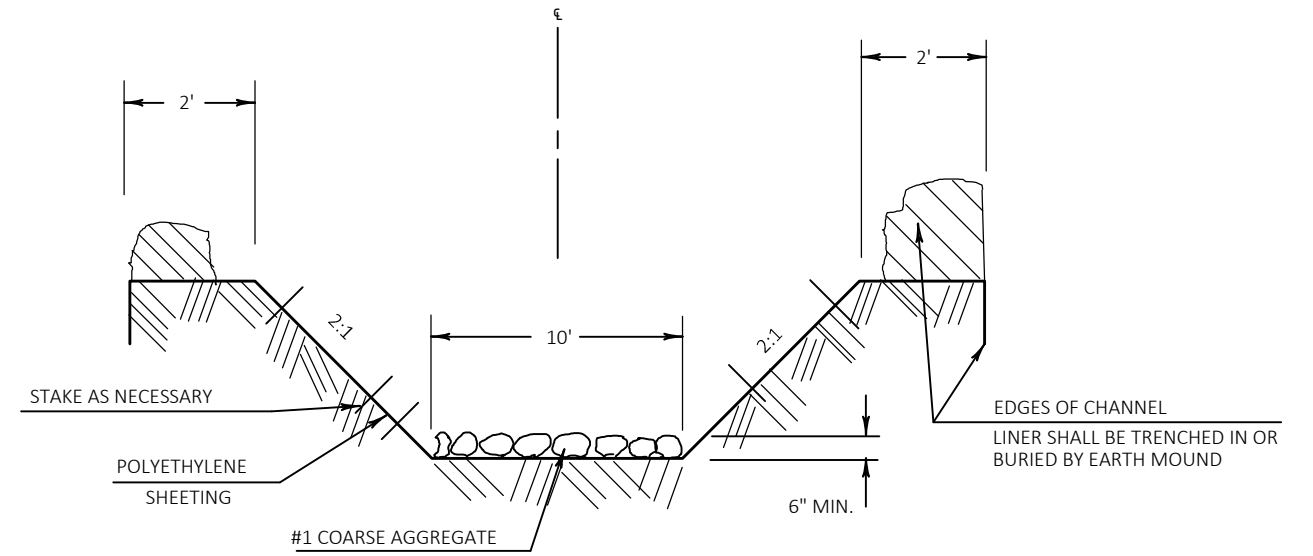
PROPOSED TYPICAL SECTION TEMPORARY BYPASS
 STA 00+10'TB1 TO 6+64'TB1
 STA 4+92'TB2 TO 16+40'TB2



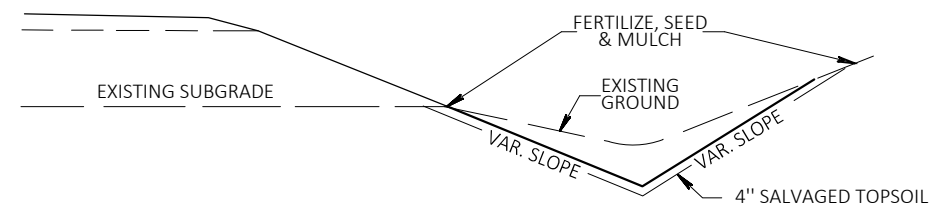
PROPOSED TYPICAL SECTION USH 12
 STA 237+11 TO 238+61
 STA 346+00 TO 348+28



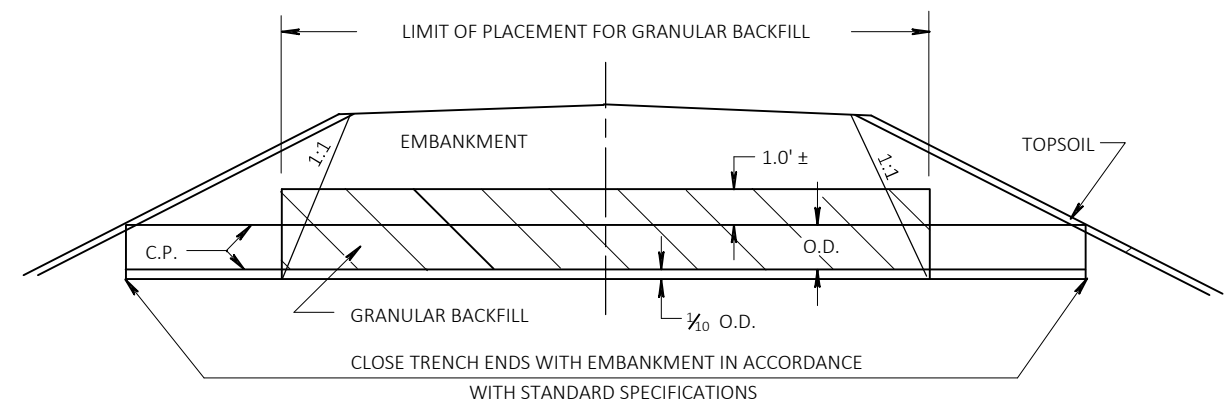
EXCLUSION FENCING TURN-AROUND



TYPICAL SECTION OF TEMPORARY CHANNEL CHANGE



DITCH SHAPING DETAIL



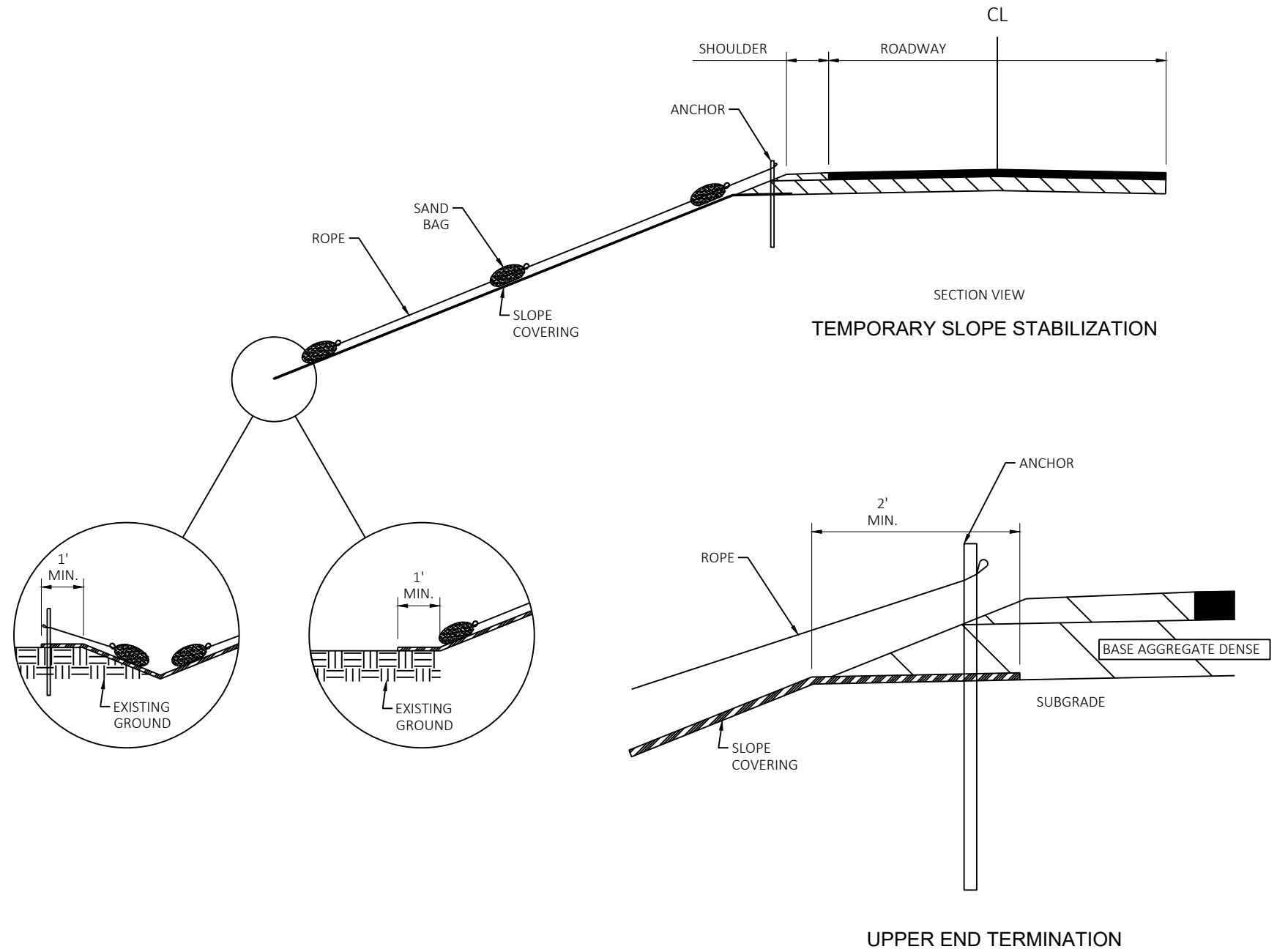
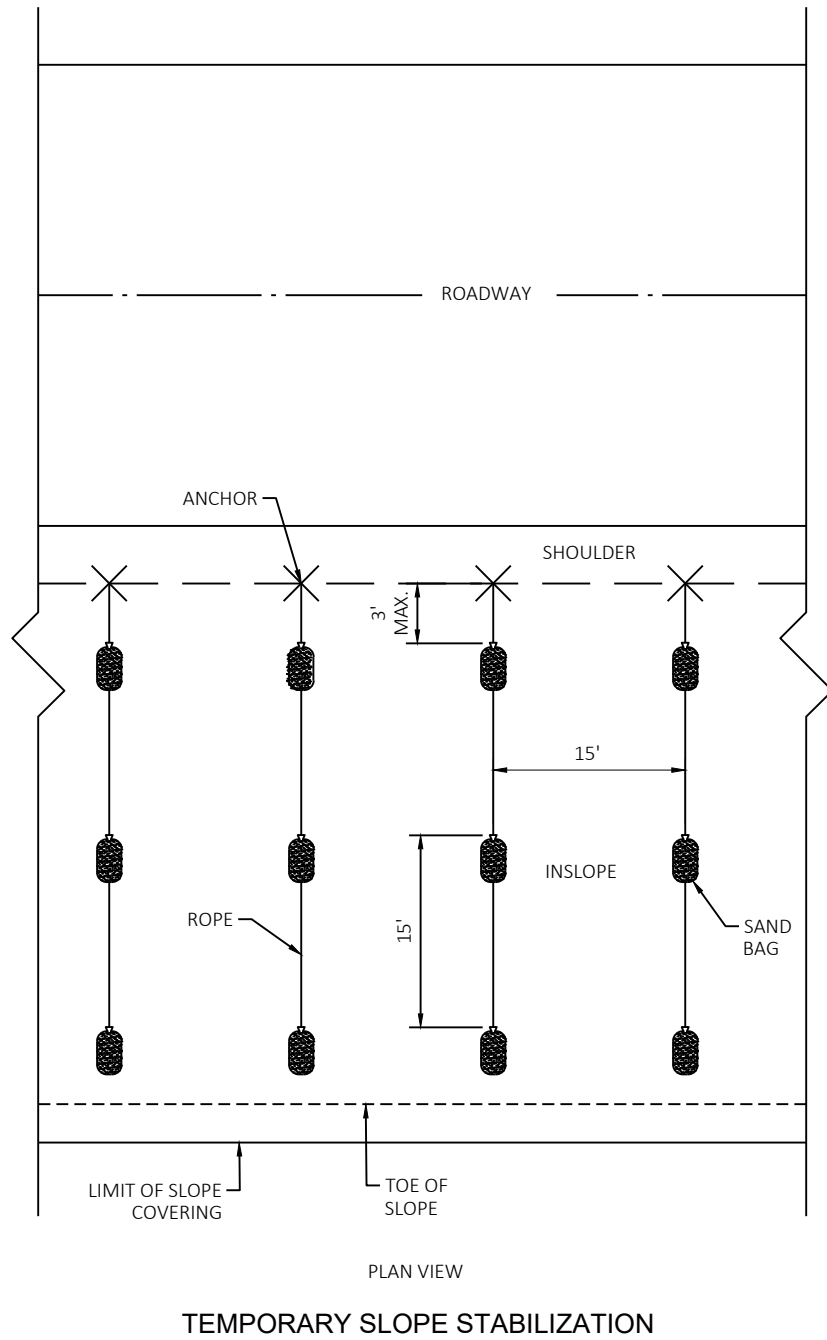
CULVERT BACKFILL DETAIL

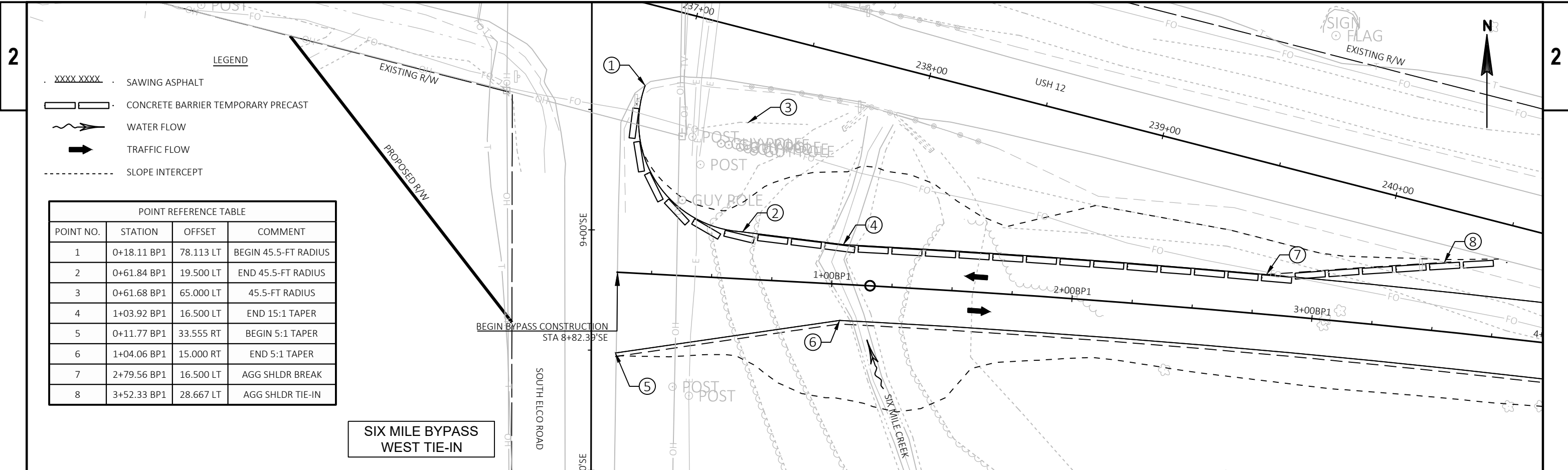
GENERAL NOTES

WHEN INSLOPE LENGTH IS NOT MULTIPLE OF 15 FEET PLACE ADDITIONAL SAND BAG WITHIN 1 FOOT OF TOE OF SLOPE.

THE UPPERMOST SAND BAG SHALL BE PLACED WITHIN 3 FEET OF THE ANCHOR. THE 15 FEET SAND BAG SPACING ALONG THE ROPE IS TO BEGIN FROM THE UPPERMOST SAND BAG.

REFER TO THE PROJECT SPECIAL PROVISIONS FOR PAYMENT AND MATERIAL SPECIFICATIONS.





LEGEND

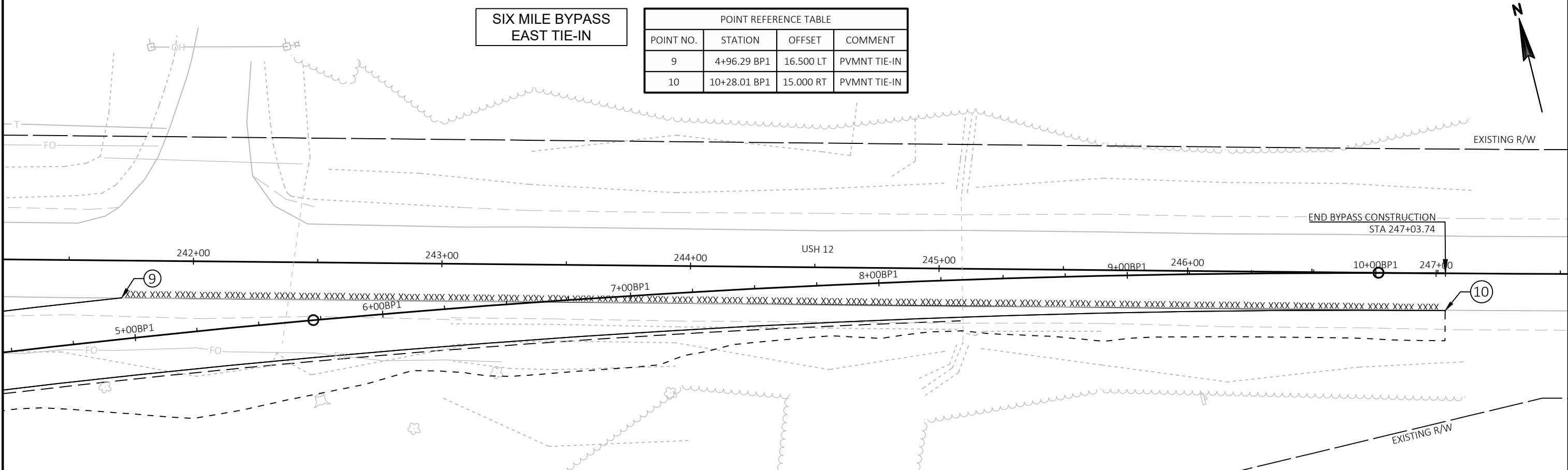
- XXXX XXXX SAWING ASPHALT
- CONCRETE BARRIER TEMPORARY PRECAST
- WATER FLOW
- TRAFFIC FLOW
- SLOPE INTERCEPT

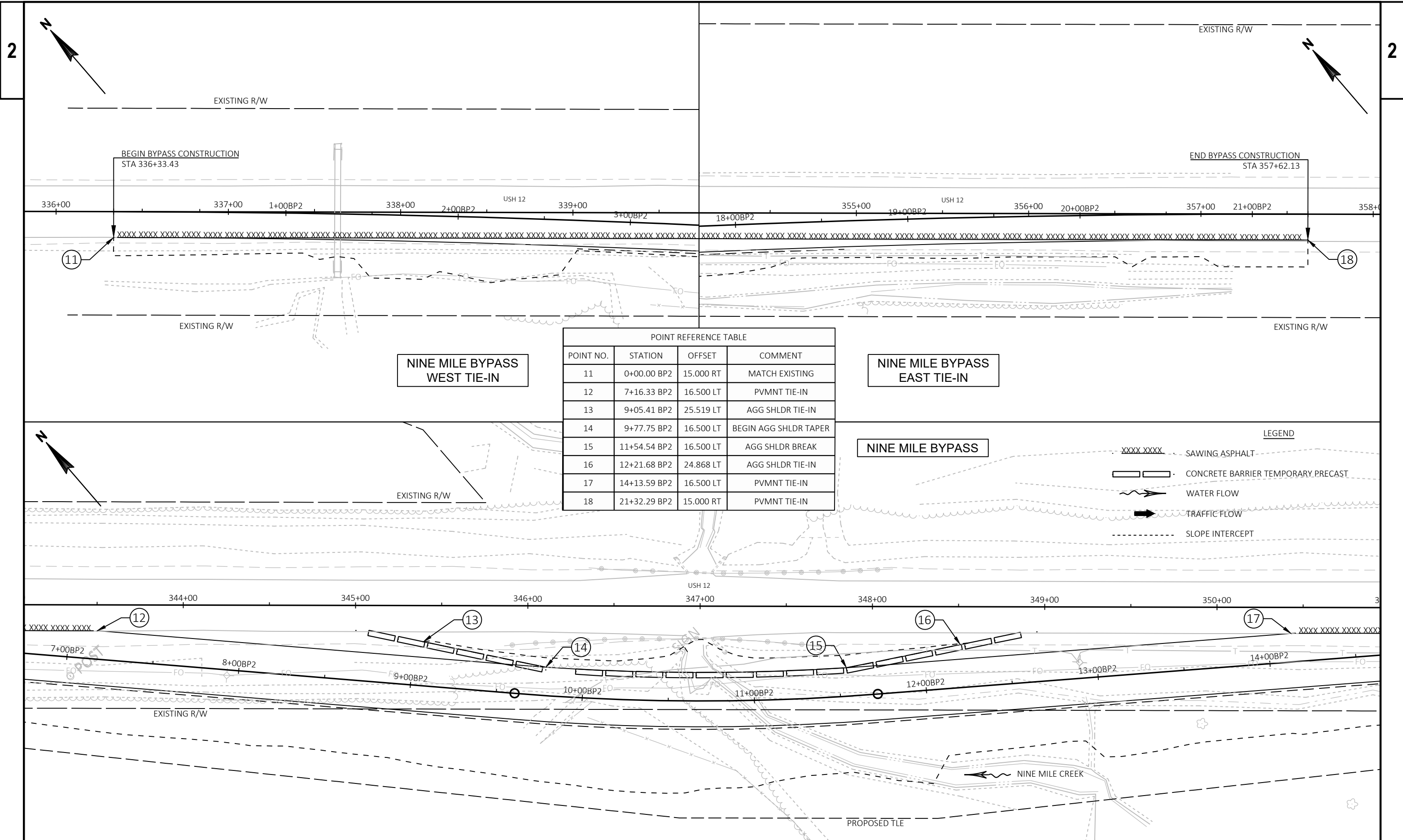
POINT REFERENCE TABLE			
POINT NO.	STATION	OFFSET	COMMENT
1	0+18.11 BP1	78.113 LT	BEGIN 45.5-FT RADIUS
2	0+61.84 BP1	19.500 LT	END 45.5-FT RADIUS
3	0+61.68 BP1	65.000 LT	45.5-FT RADIUS
4	1+03.92 BP1	16.500 LT	END 15:1 TAPER
5	0+11.77 BP1	33.555 RT	BEGIN 5:1 TAPER
6	1+04.06 BP1	15.000 RT	END 5:1 TAPER
7	2+79.56 BP1	16.500 LT	AGG SHLDR BREAK
8	3+52.33 BP1	28.667 LT	AGG SHLDR TIE-IN

SIX MILE BYPASS WEST TIE-IN

SIX MILE BYPASS EAST TIE-IN

POINT REFERENCE TABLE			
POINT NO.	STATION	OFFSET	COMMENT
9	4+96.29 BP1	16.500 LT	PVMNT TIE-IN
10	10+28.01 BP1	15.000 RT	PVMNT TIE-IN





POINT REFERENCE TABLE

POINT NO.	STATION	OFFSET	COMMENT
11	0+00.00 BP2	15.000 RT	MATCH EXISTING
12	7+16.33 BP2	16.500 LT	PVMNT TIE-IN
13	9+05.41 BP2	25.519 LT	AGG SHLDR TIE-IN
14	9+77.75 BP2	16.500 LT	BEGIN AGG SHLDR TAPER
15	11+54.54 BP2	16.500 LT	AGG SHLDR BREAK
16	12+21.68 BP2	24.868 LT	AGG SHLDR TIE-IN
17	14+13.59 BP2	16.500 LT	PVMNT TIE-IN
18	21+32.29 BP2	15.000 RT	PVMNT TIE-IN

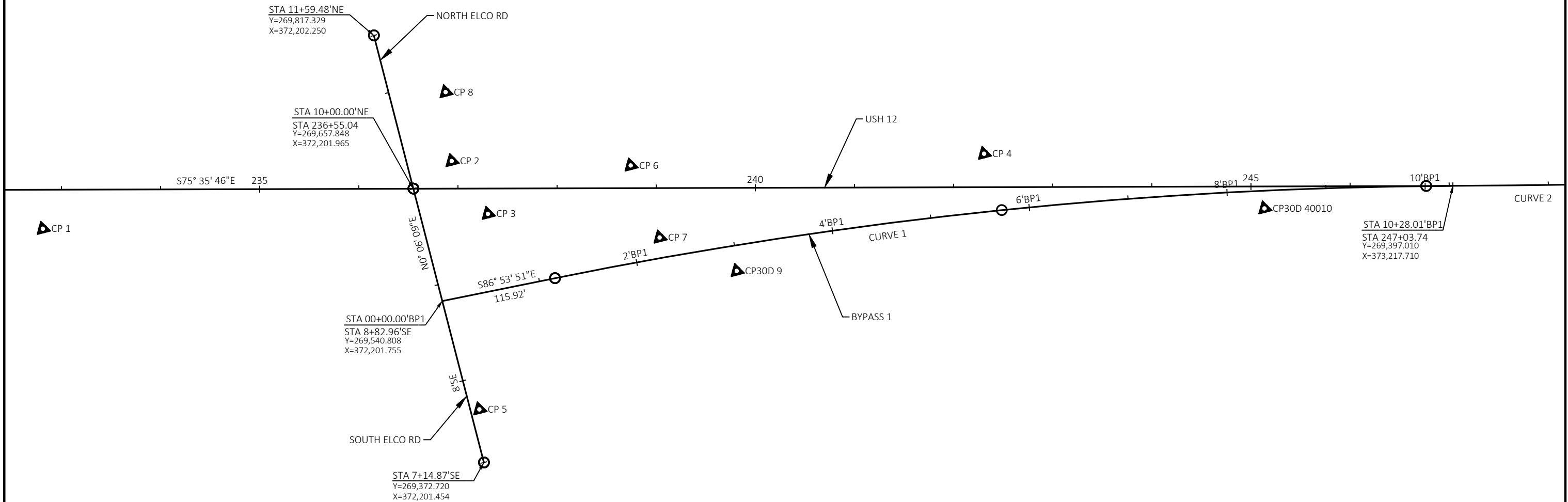
- LEGEND
- XXXX XXXX SAWING ASPHALT
 - CONCRETE BARRIER TEMPORARY PRECAST
 - WATER FLOW
 - TRAFFIC FLOW
 - SLOPE INTERCEPT

Six Mile Creek Bypass					
SUPERELEVATION TRANSITION EVENT POINTS		RATE			
LOCATION	STATION	LEFT OF CROWNLIN		RIGHT OF CROWNLIN	
		LEFT SHOULDER	LEFT LANE	RIGHT LANE	RIGHT SHOULDER
Curve 1					
Begin Bypass	0+10.83'BP1				
EndNormalShoulder	0+42.58'BP1	-4.00%	-2.00%	-2.00%	-4.00%
EndNormalCrown	0+42.58'BP1	-4.00%	-2.00%	-2.00%	-2.00%
LevelCrown	0+86.58'BP1	.00%	.00%	-2.00%	-2.00%
BeginFullSuper	1+30.58'BP1	2.00%	2.00%	-2.00%	-2.00%
ReverseCrown	1+30.58'BP1	2.00%	2.00%	-2.00%	-2.00%
EndFullSuper	10+13.35'BP1	2.00%	2.00%	-2.00%	-2.00%
ReverseCrown	10+13.35'BP1	2.00%	2.00%	-2.00%	-2.00%
End Bypass	10+28.01'BP1				

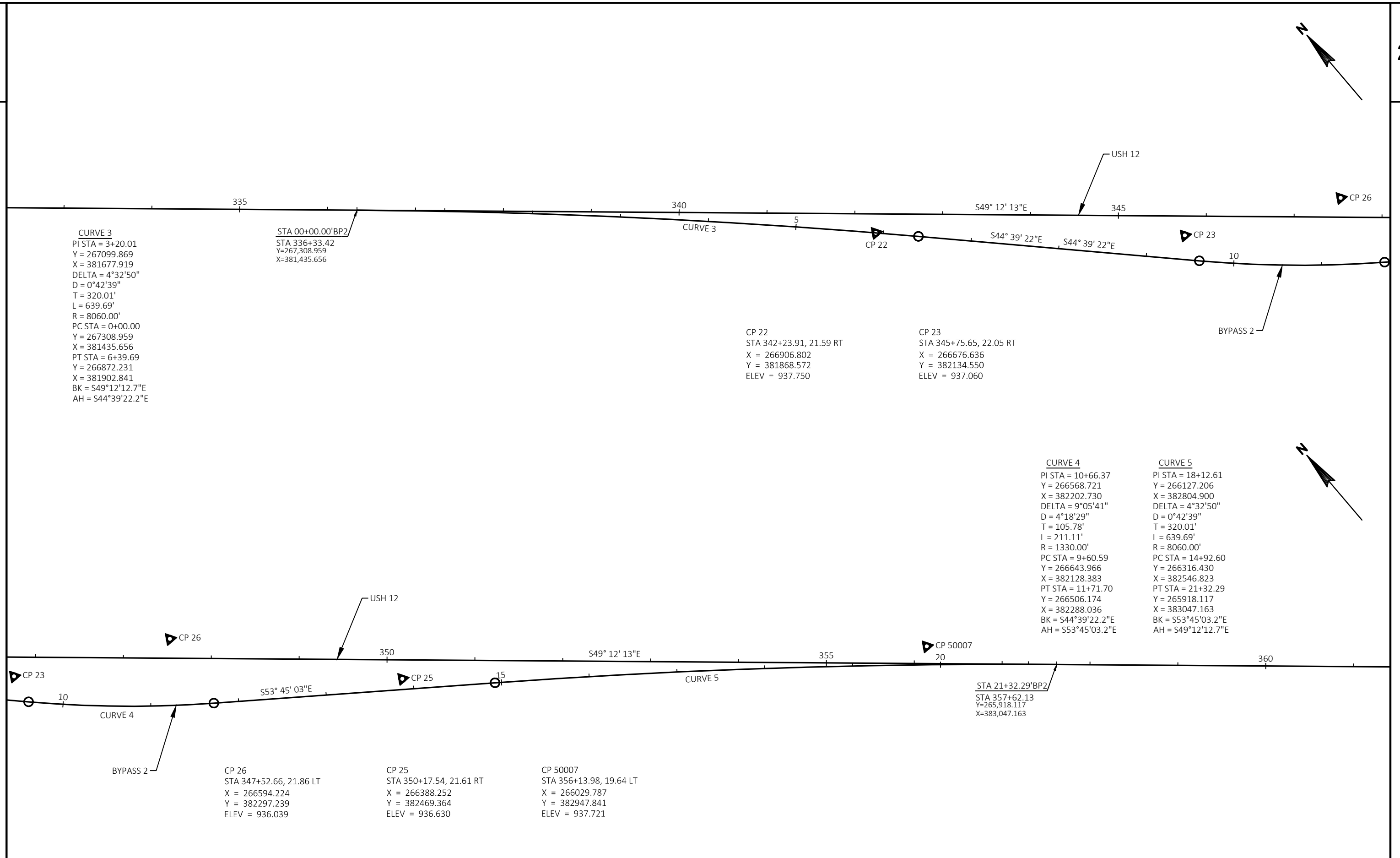
Nine Mile Creek Bypass					
SUPERELEVATION TRANSITION EVENT POINTS		RATE			
LOCATION	STATION	LEFT OF CROWNLIN		RIGHT OF CROWNLIN	
		LEFT SHOULDER	LEFT LANE	RIGHT LANE	RIGHT SHOULDER
Curve 1					
BeginBypass	0+00.00'BP2				
BeginFullSuper	0+17.66'BP2	2.00%	2.00%	-2.00%	-2.00%
ReverseCrown	0+17.66'BP2	2.00%	2.00%	-2.00%	-2.00%
EndFullSuper	6+22.03'BP2	2.00%	2.00%	-2.00%	-2.00%
ReverseCrown	6+22.03'BP2	2.00%	2.00%	-2.00%	-2.00%
LevelCrown	6+75.03'BP2	.00%	.00%	-2.00%	-2.00%
BeginNormalCrown	7+28.03'BP2	-4.00%	-2.00%	-2.00%	-2.00%
BeginNormalShoulder	7+28.03'BP2	-4.00%	-2.00%	-2.00%	-4.00%
Curve 2					
EndNormalShoulder	8+00.58'BP2	-4.00%	-2.00%	-2.00%	-4.00%
EndNormalCrown	8+00.58'BP2	-2.00%	-2.00%	-2.00%	-4.00%
LevelCrown	8+53.92'BP2	-2.00%	-2.00%	.00%	.00%
ReverseCrown	9+07.25'BP2	-2.00%	-2.00%	2.00%	2.00%
BeginFullSuper	10+13.92'BP2	-6.00%	-6.00%	6.00%	6.00%
EndFullSuper	11+18.38'BP2	-6.00%	-6.00%	6.00%	6.00%
ReverseCrown	12+25.04'BP2	-2.00%	-2.00%	2.00%	2.00%
LevelCrown	12+78.38'BP2	-2.00%	-2.00%	.00%	.00%
BeginNormalCrown	13+31.71'BP2	-2.00%	-2.00%	-2.00%	-4.00%
BeginNormalShoulder	13+31.71'BP2	-4.00%	-2.00%	-2.00%	-4.00%
Curve 3					
EndNormalShoulder	14+04.26'BP2	-4.00%	-2.00%	-2.00%	-4.00%
EndNormalCrown	14+04.26'BP2	-4.00%	-2.00%	-2.00%	-2.00%
LevelCrown	14+57.26'BP2	.00%	.00%	-2.00%	-2.00%
BeginFullSuper	15+10.26'BP2	2.00%	2.00%	-2.00%	-2.00%
ReverseCrown	15+10.26'BP2	2.00%	2.00%	-2.00%	-2.00%
EndFullSuper	21+14.63'BP2	2.00%	2.00%	-2.00%	-2.00%
ReverseCrown	21+14.63'BP2	2.00%	2.00%	-2.00%	-2.00%
End Bypass	21+32.29'BP2				



PI STA = 5+73.41'BP1 Y = 269509.772 X = 372774.327 DELTA = 11°09'59" D = 1°13'27" T CURVE 1 L = 912.10' R = 4680.00' PC STA = 1+15.92'BP1 Y = 269534.534 X = 372317.501 PT STA = 10+28.01'BP1 Y = 269397.010 X = 373217.710 BK = S86°53'50.6"E AH = S75°43'51.2"E	PI STA = 269+78.70 Y = 268831.073 X = 375421.153 DELTA = 22°43'00" D = 0°30'00" T CURVE 2 L = 4543.33' R = 11459.19' PC STA = 246+76.80 Y = 269403.680 X = 373191.611 PT STA = 292+20.13 Y = 269163.874 X = 377698.867 BK = S75°35'46.3"E AH = N81°41'14.0"E
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CP 1 STA 232+81.18, 39.53 RT X = 269712.556 Y = 371830.028 ELEV = 914.650	CP 2 STA 236+93.75, 27.74 LT X = 269675.092 Y = 372246.357 ELEV = 917.100	CP 3 STA 237+30.17, 25.62 RT X = 269614.345 Y = 372268.357 ELEV = 916.790	CP 4 STA 242+30.83, 33.62 LT X = 269547.177 Y = 372768.015 ELEV = 920.640	CP 5 STA 237+21.05, 222.73 RT X = 269425.697 Y = 372210.495 ELEV = 916.690	CP 6 STA 238+74.29, 22.79 LT X = 269625.380 Y = 372419.992 ELEV = 918.300	CP 7 STA 239+03.32, 49.95 RT X = 269547.704 Y = 372430.017 ELEV = 917.940	CP 8 STA 236+87.78, 97.19 LT X = 269743.843 Y = 372257.850 ELEV = 912.530	CP30D 9 STA 239+81.01, 83.91 RT X = 269495.491 Y = 372496.817 ELEV = 917.719	CP30D 40010 STA 245+13.51, 22.19 RT X = 269422.803 Y = 373027.935 ELEV = 921.444
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CURVE 3
 PI STA = 3+20.01
 Y = 267099.869
 X = 381677.919
 DELTA = 4°32'50"
 D = 0°42'39"
 T = 320.01'
 L = 639.69'
 R = 8060.00'
 PC STA = 0+00.00
 Y = 267308.959
 X = 381435.656
 PT STA = 6+39.69
 Y = 266872.231
 X = 381902.841
 BK = S49°12'12.7"E
 AH = S44°39'22.2"E

STA 00+00.00'BP2
 STA 336+33.42
 Y=267,308.959
 X=381,435.656

CP 22
 STA 342+23.91, 21.59 RT
 X = 266906.802
 Y = 381868.572
 ELEV = 937.750

CP 23
 STA 345+75.65, 22.05 RT
 X = 266676.636
 Y = 382134.550
 ELEV = 937.060

CURVE 4
 PI STA = 10+66.37
 Y = 266568.721
 X = 382202.730
 DELTA = 9°05'41"
 D = 4°18'29"
 T = 105.78'
 L = 211.11'
 R = 1330.00'
 PC STA = 9+60.59
 Y = 266643.966
 X = 382128.383
 PT STA = 11+71.70
 Y = 266506.174
 X = 382288.036
 BK = S44°39'22.2"E
 AH = S53°45'03.2"E

CURVE 5
 PI STA = 18+12.61
 Y = 266127.206
 X = 382804.900
 DELTA = 4°32'50"
 D = 0°42'39"
 T = 320.01'
 L = 639.69'
 R = 8060.00'
 PC STA = 14+92.60
 Y = 266316.430
 X = 382546.823
 PT STA = 21+32.29
 Y = 265918.117
 X = 383047.163
 BK = S53°45'03.2"E
 AH = S49°12'12.7"E

STA 21+32.29'BP2
 STA 357+62.13
 Y=265,918.117
 X=383,047.163

CP 26
 STA 347+52.66, 21.86 LT
 X = 266594.224
 Y = 382297.239
 ELEV = 936.039

CP 25
 STA 350+17.54, 21.61 RT
 X = 266388.252
 Y = 382469.364
 ELEV = 936.630






CP 50007
 STA 356+13.98, 19.64 LT
 X = 266029.787
 Y = 382947.841
 ELEV = 937.721

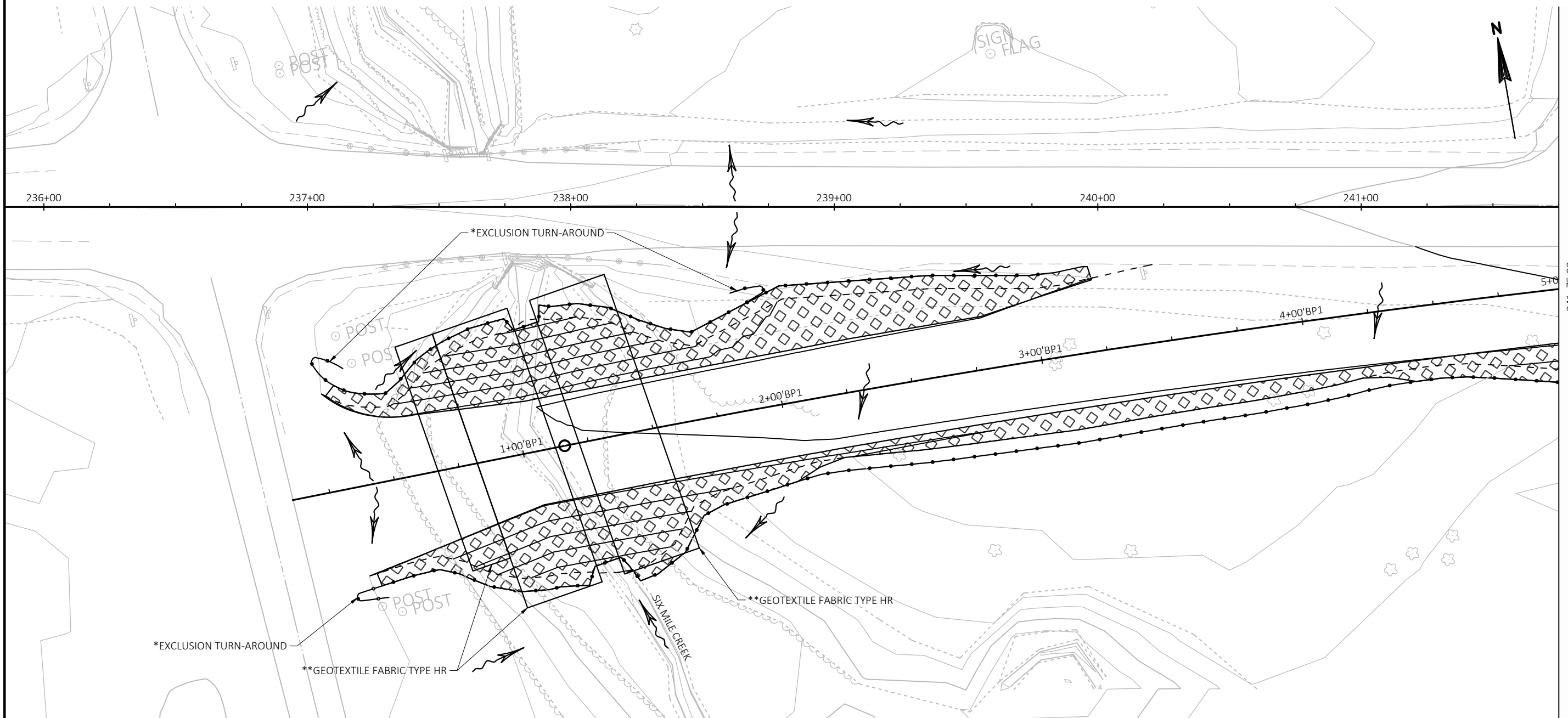
*NOTE: PROVIDE EXCLUSION FENCING TURN-AROUND AT FENCE END PER CONSTRUCTION DETAIL. TO REMAIN FOR DURATION OF PROJECT.

**NOTE: PRIOR TO PLACING FILL FOR TEMPORARY BYPASS, INSTALL GEOTEXTILE FABRIC TYPE HR ACCORDING TO PLAN OR AS DIRECTED BY THE ENGINEER. THE PURPOSE OF THIS FABRIC IS FOR WETLAND PROTECTION. REMOVAL OF THE GEOTEXTILE FABRIC TYPE HR IS INCIDENTAL TO EXCAVATION COMMON.

REFER TO THE TEMPORARY SLOPE STABILIZATION CONSTRUCTION DETAIL FOR INSTALLATION.

LEGEND

-  SILT FENCE
-  RIP RAP OR STONE DITCH CHECK
-  SURFACE WATER FLOW
-  TEMPORARY SLOPE STABILIZATION
-  SLOPE INTERCEPT








PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	EROSION CONTROL - TEMPORARY	SHEET	E
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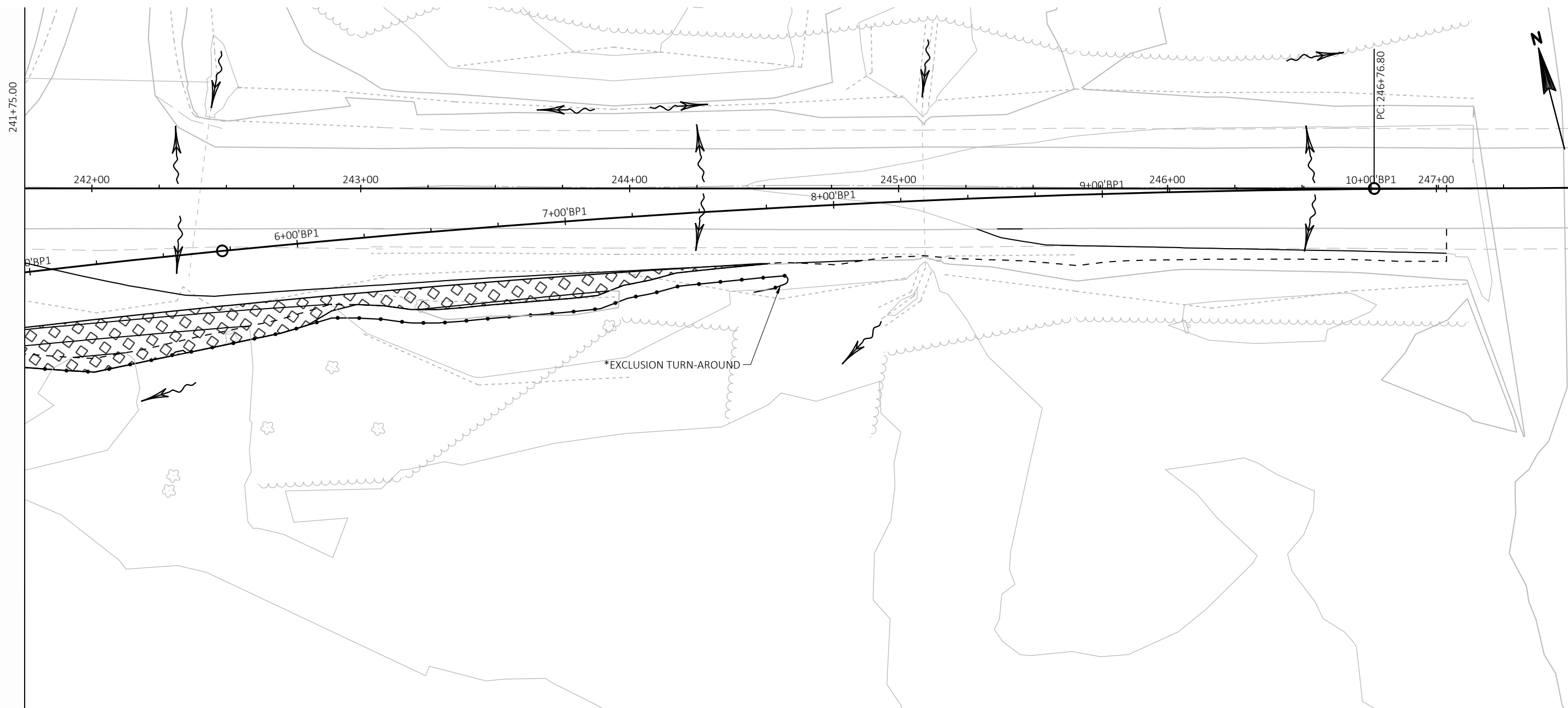
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LEGEND

-  SILT FENCE
-  RIP RAP OR STONE DITCH CHECK
-  SURFACE WATER FLOW
-  TEMPORARY SLOPE STABILIZATION
-  SLOPE INTERCEPT



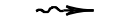

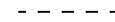


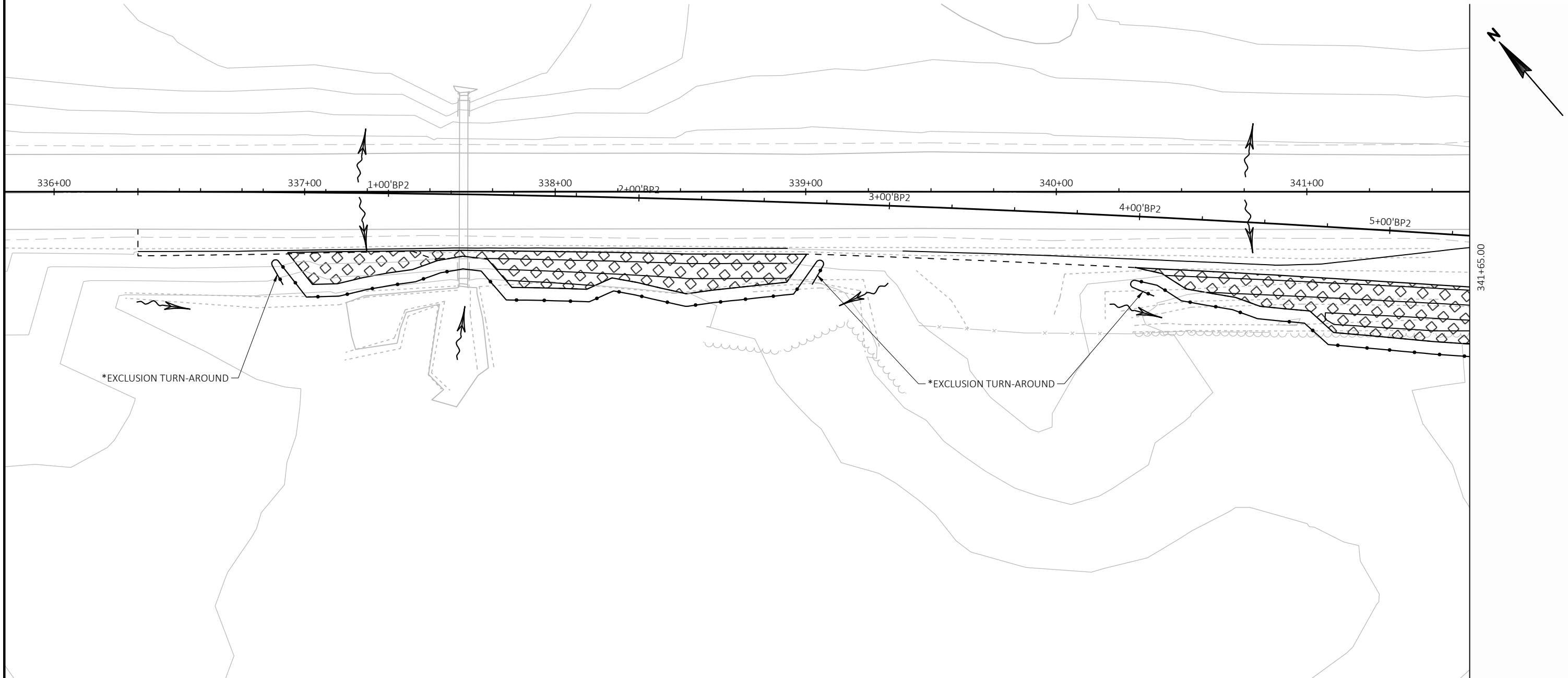
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LEGEND

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-  SURFACE WATER FLOW
-  TEMPORARY SLOPE STABILIZATION
-  SLOPE INTERCEPT





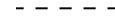


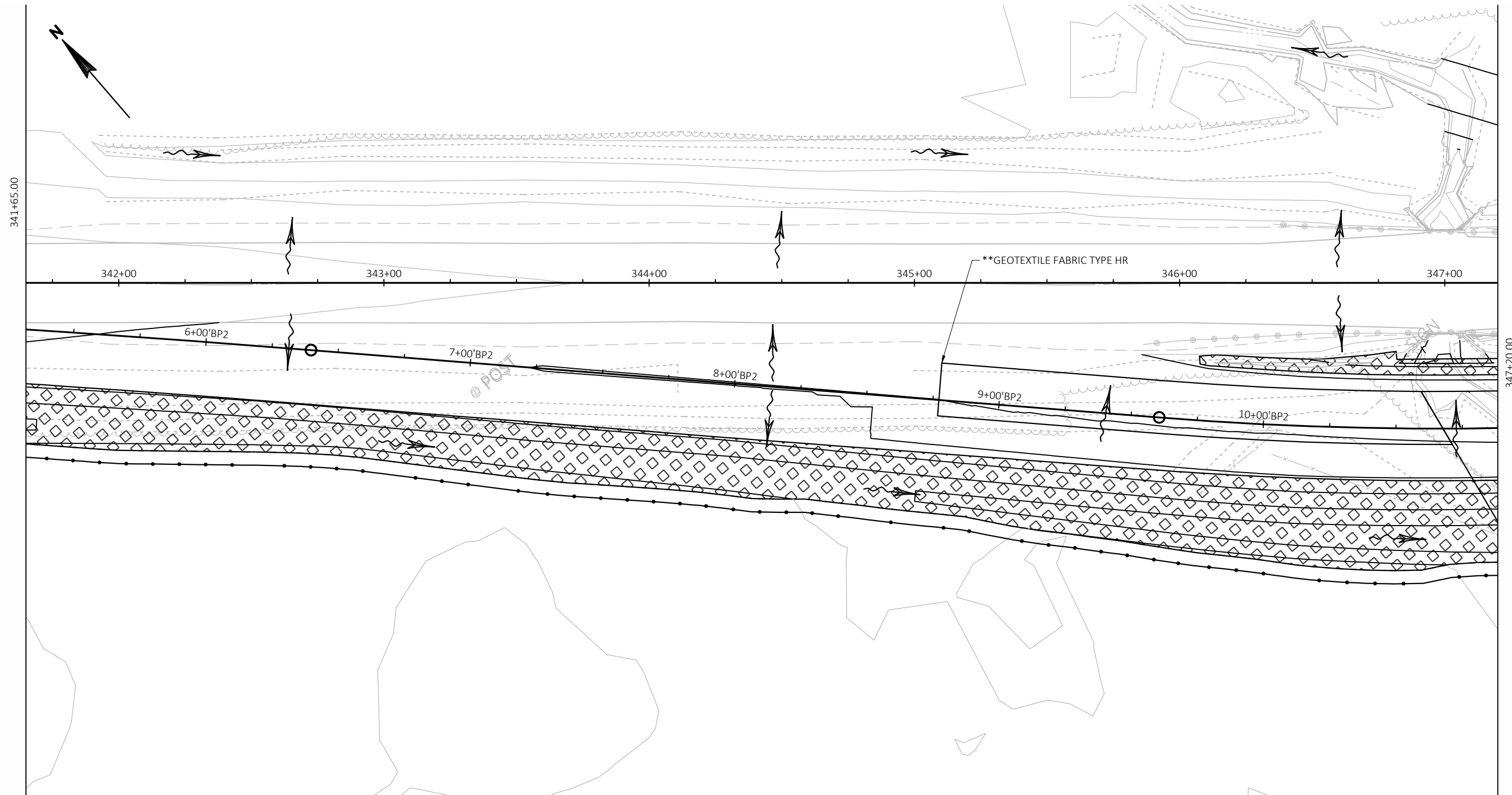
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


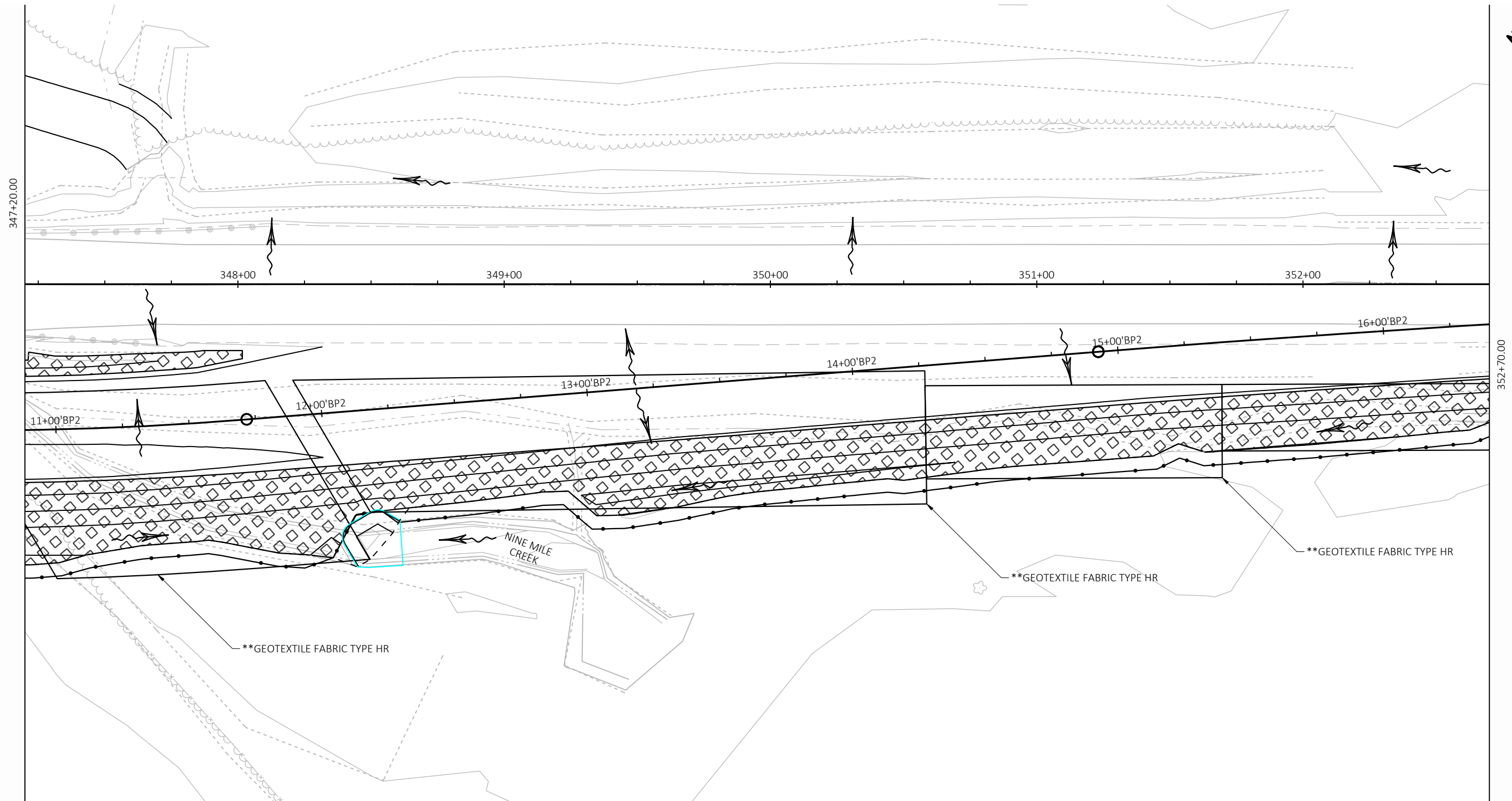
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-  SILT FENCE
-  RIP RAP OR STONE DITCH CHECK
-  SURFACE WATER FLOW
-  TEMPORARY SLOPE STABILIZATION
-  SLOPE INTERCEPT





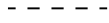


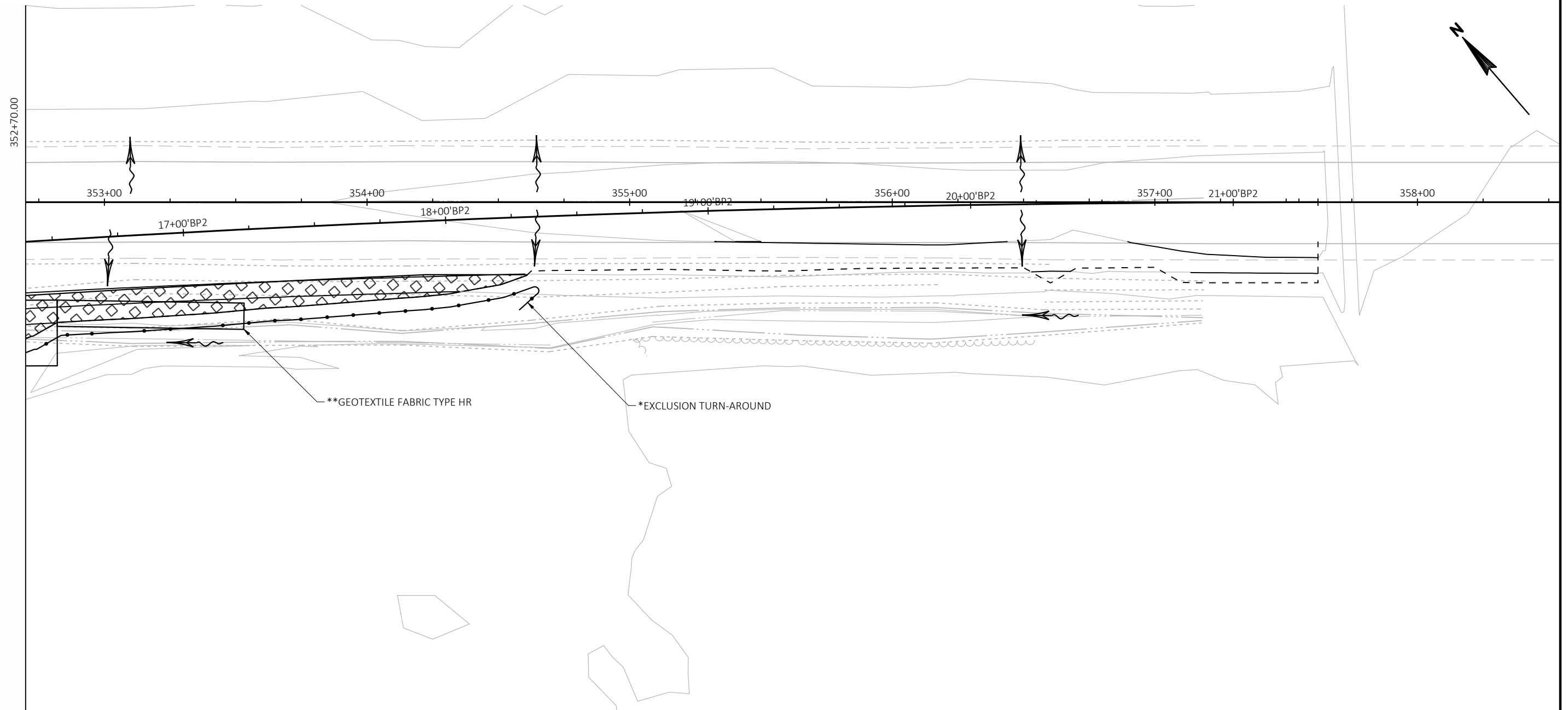
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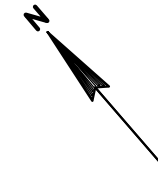
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LEGEND

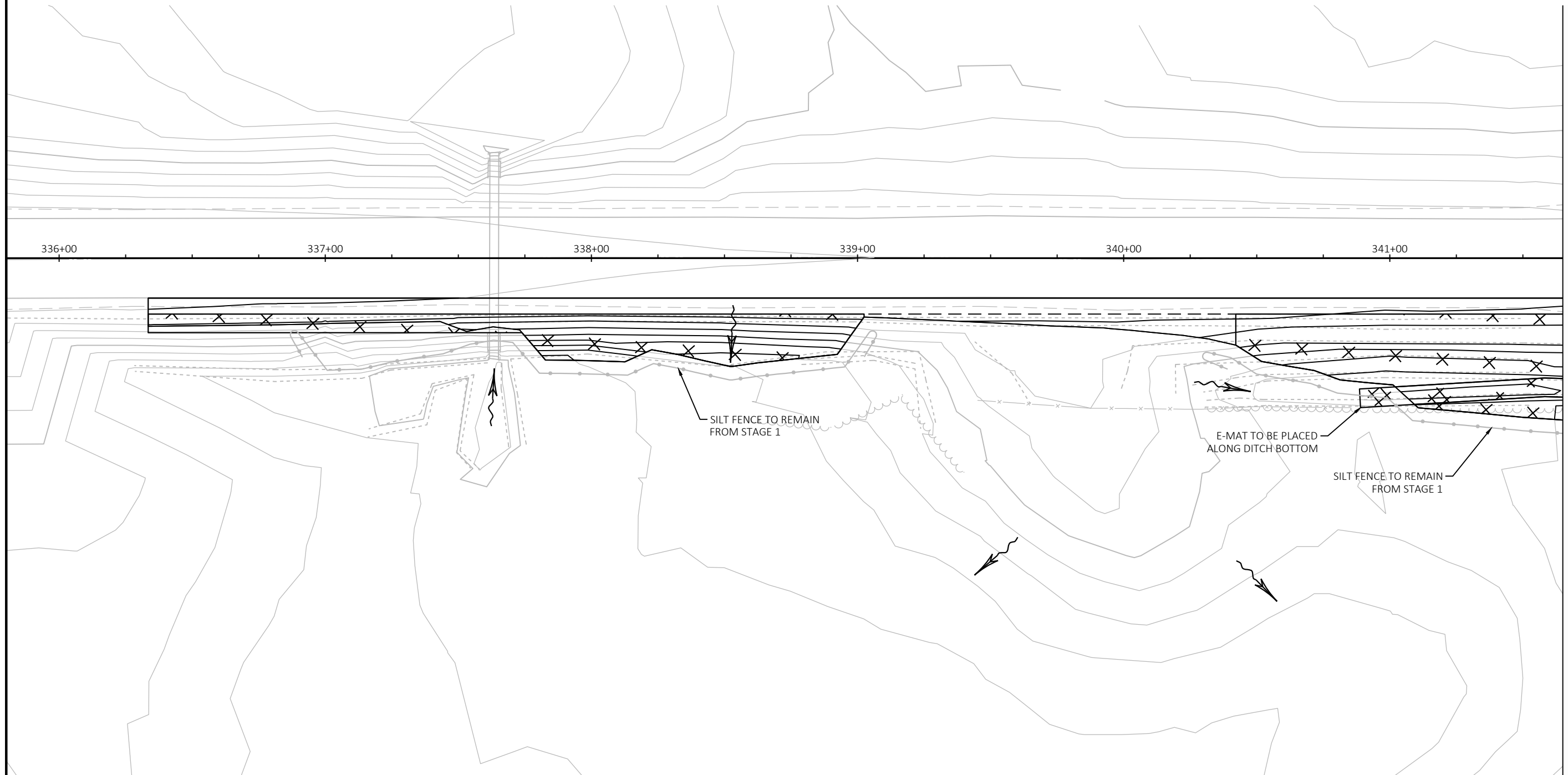
-  SILT FENCE
-  RIP RAP OR STONE DITCH CHECK
-  SURFACE WATER FLOW
-  TEMPORARY SLOPE STABILIZATION
-  SLOPE INTERCEPT



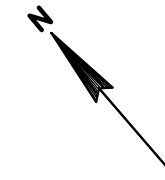
NOTE: RESTORATION ITEMS NOT TO BE USED IN THE WETLAND AREAS.



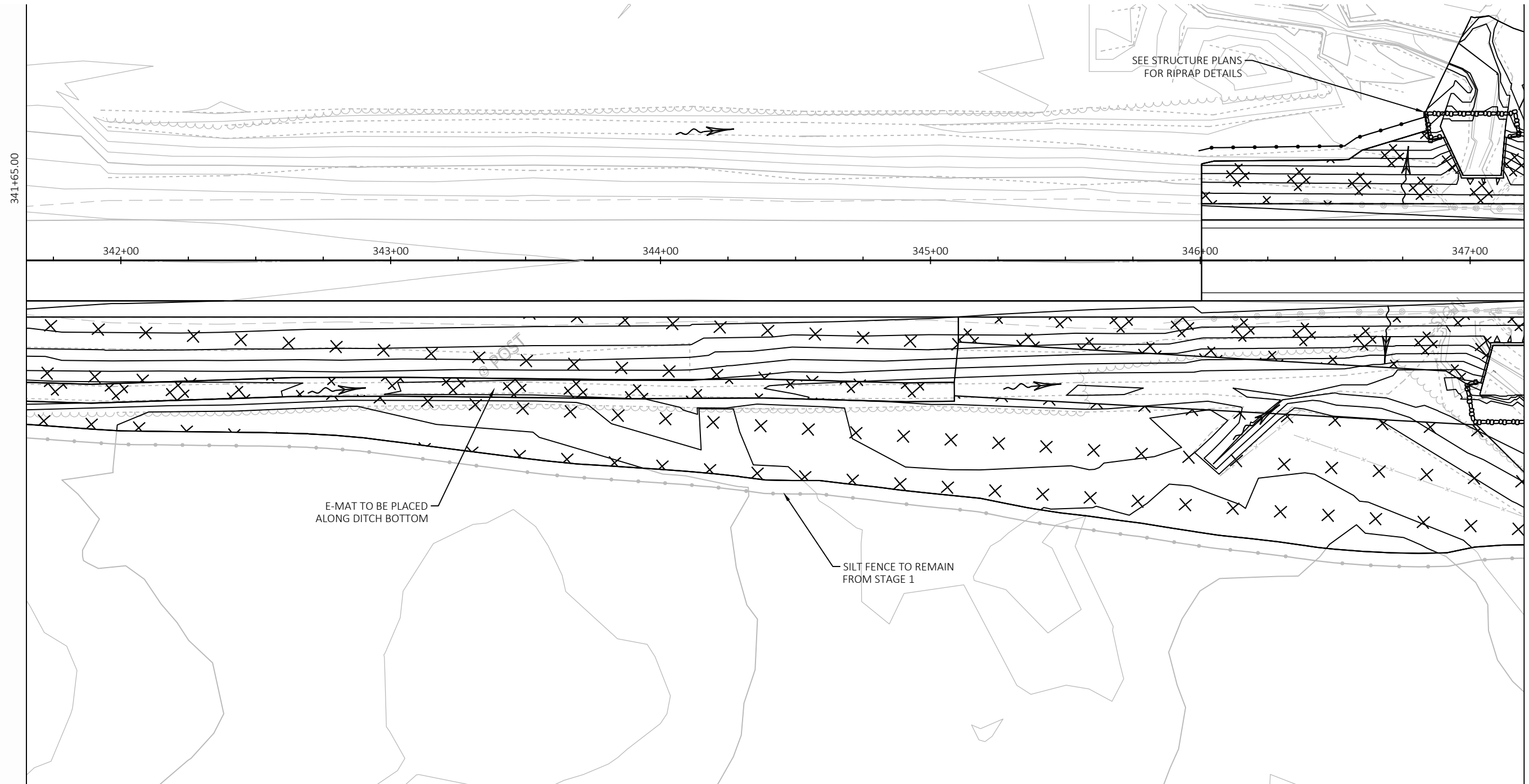
LEGEND	
#####	EROSION MAT URBAN CLASS I, TYPE B
+++++	MULCH
—●—●—●—●—●—	SILT FENCE
—○—○—○—○—○—	RIP RAP
~>	SURFACE WATER FLOW



NOTE: RESTORATION ITEMS NOT TO BE USED IN THE WETLAND AREAS.



LEGEND	
#####	EROSION MAT URBAN CLASS I, TYPE B
+++++	MULCH
—●—●—●—●—●—	SILT FENCE
—○—○—○—○—○—	RIP RAP
~>	SURFACE WATER FLOW



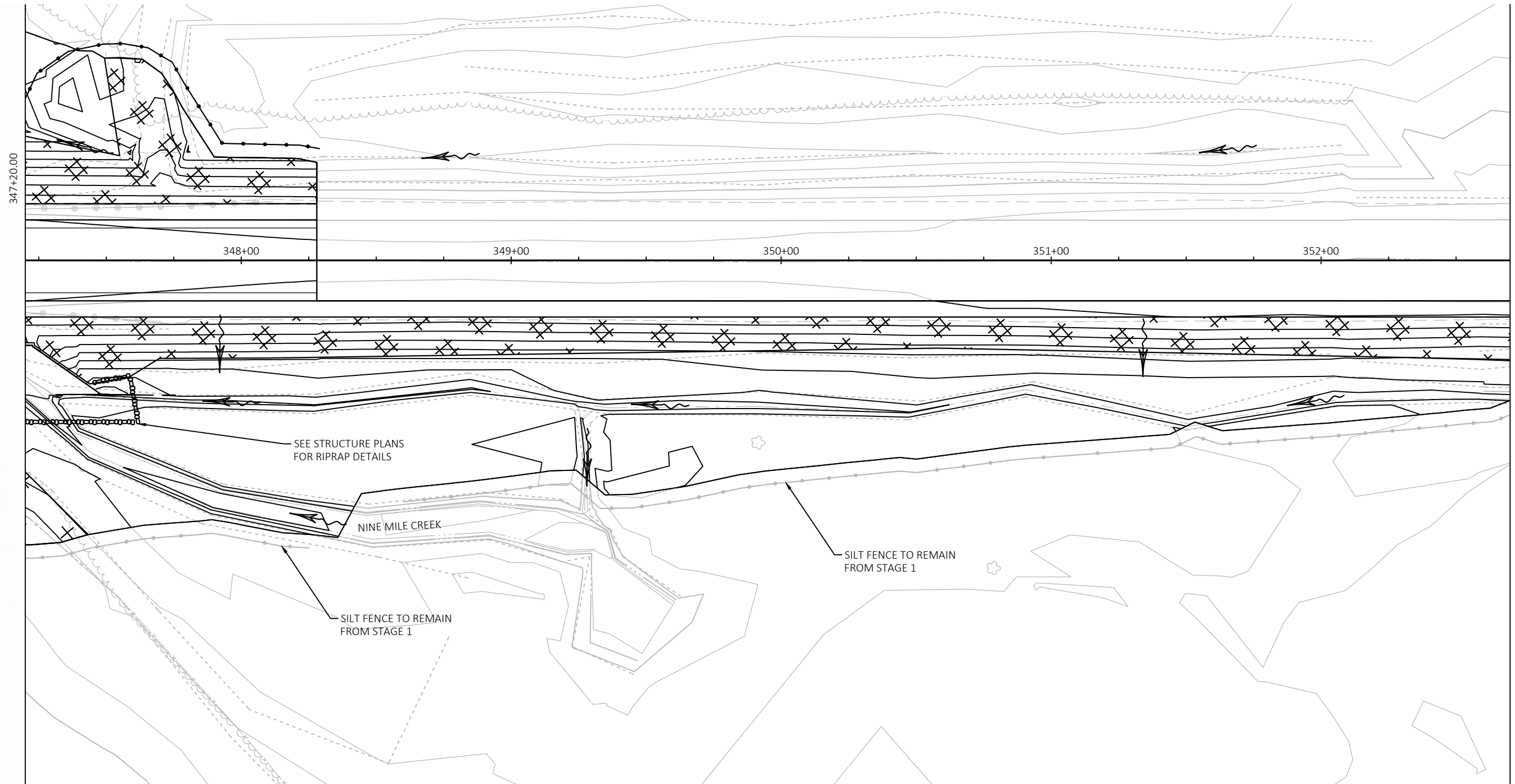
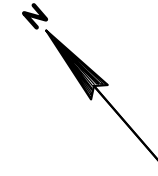
PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	EROSION CONTROL - PERMANENT	SHEET	E
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NOTE: RESTORATION ITEMS NOT TO BE USED IN THE WETLAND AREAS.

2

2

LEGEND	
#####	EROSION MAT URBAN CLASS I, TYPE B
+++++	MULCH
—●—●—●—●—●—	SILT FENCE
—○—○—○—○—○—	RIP RAP
~>	SURFACE WATER FLOW



PROJECT NO: 7080-01-72

HWY: USH 12

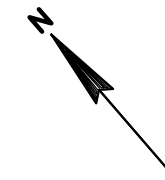
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EROSION CONTROL - PERMANENT

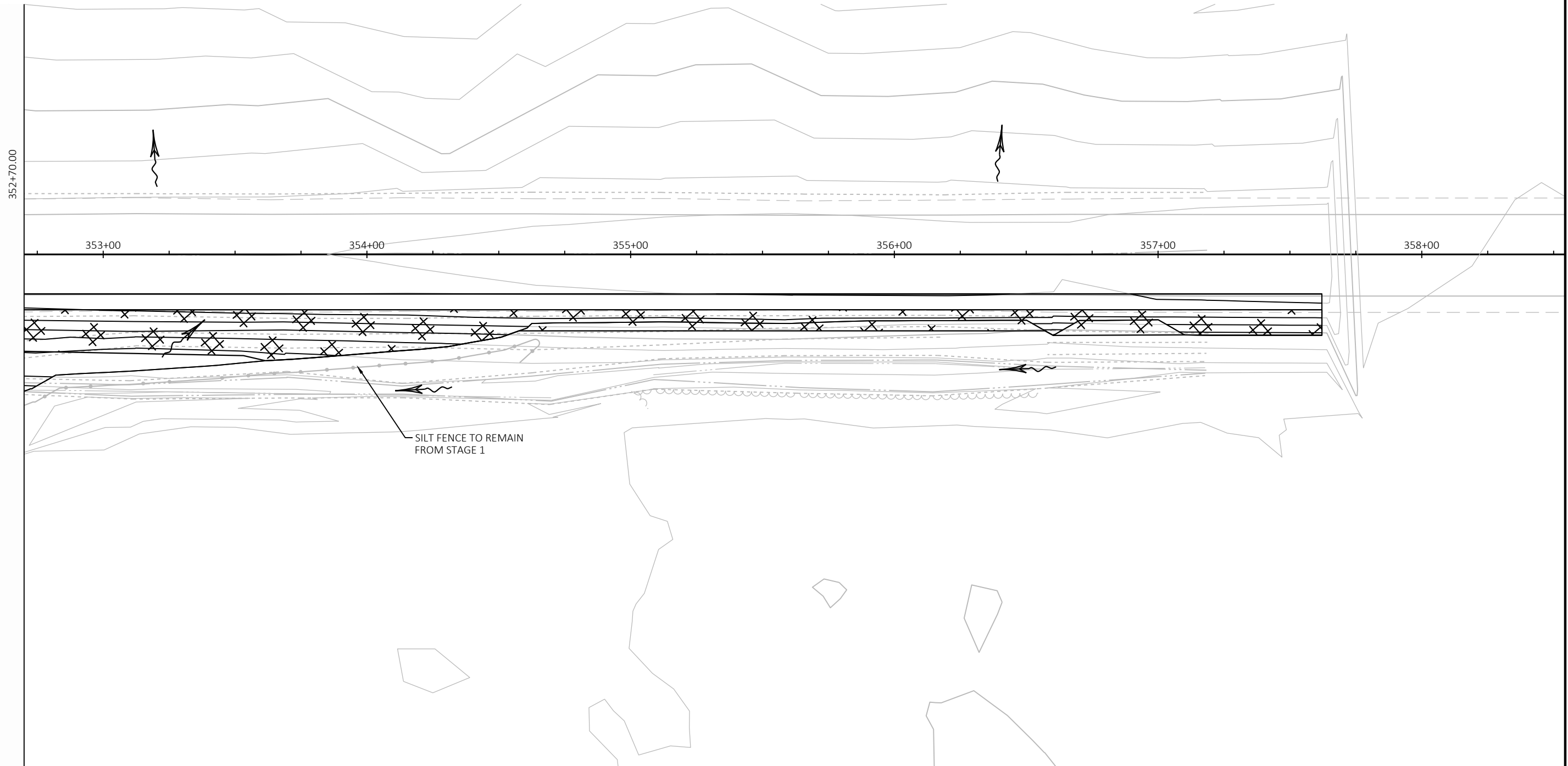
SHEET

E

NOTE: RESTORATION ITEMS NOT TO BE USED IN THE WETLAND AREAS.



LEGEND	
#####	EROSION MAT URBAN CLASS I, TYPE B
+++++	MULCH
—●—●—●—●—●—	SILT FENCE
—○—○—○—○—○—	RIP RAP
~>	SURFACE WATER FLOW



SILT FENCE TO REMAIN FROM STAGE 1

GENERAL NOTES FOR TRAFFIC CONTROL

FOR TRAFFIC CONTROL OF STAGE 1 AND STAGE 3 OF CONSTRUCTION SEE S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION", AND "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY".

THE EXACT NUMBER, LOCATION, AND SPACING OF SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

A FLAGGER MAY BE REQUIRED WHERE CONSTRUCTION VEHICLES ENTER OR LEAVE WORK AREAS IF WARRANTED BY CONDITIONS OR AS DIRECTED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

EXISTING AND ADVANCE WARNING TRAFFIC SIGNS MAY REQUIRE RELOCATION DURING CONSTRUCTION STAGING AND ARE INCIDENTAL TO THE CONTRACT.

PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) ARE TO BE PLACED AT PLAN LOCATIONS ONE WEEK PRIOR TO THE START OF CONSTRUCTION.

TRAFFIC CONTROL SIGNS PCMS MESSAGES		
	PRIOR TO CONSTRUCTION	
PCMS LOCATION	PHASE 1 (2 SEC)	PHASE 2 (2 SEC)
USH 12 EB 0.1 MILE WEST OF ELCO RD	ROAD WORK TO START	DATE S. ELCO CLOSED
USH 12 WB 0.1 MILE EAST OF ELCO RD	ROAD WORK TO START	DATE S. ELCO CLOSED

CONSTRUCTION STAGING OPERATIONS

STAGE 1

CONSTRUCTION OPERATIONS:

- INSTALL TEMPORARY CULVERT PIPES FOR C-18-0049 & C-18-0050 TEMPORARY DIVERSION CHANNELS
- CONSTRUCT TEMPORARY BYPASS FOR C-18-0049 & C-18-0050

TRAFFIC OPERATIONS:

- TRAFFIC WILL BE CONTROLLED BY FLAGGING OPERATIONS AND SHOULDER CLOSURES ON USH 12

STAGE 2

CONSTRUCTION OPERATIONS:

- CONSTRUCT PANELS AND NORTH END WINGS OF C-18-0049 & C-18-0050 (REFER TO STRUCTURE PLANS)
- CONSTRUCT ROADWAY APPROACHES AT C-18-0049 & C-18-0050

TRAFFIC OPERATIONS:

- BIDIRECTIONAL TRAFFIC WILL BE MAINTAINED USING THE TEMPORARY BYPASSES

STAGE 3

CONSTRUCTION OPERATIONS - STAGE 3A:

- COMPLETE SOUTH END WINGS OF C-18-0049 & C-18-0050 (REFER TO STRUCTURE PLANS)

CONSTRUCTION OPERATIONS - STAGE 3B:

- REMOVE TEMPORARY CULVERT PIPES FOR C-18-0049 & C-18-0050 TEMPORARY DIVERSION CHANNELS




CONSTRUCTION OPERATIONS - STAGE 3C:

- REMOVE TEMPORARY BYPASSES FOR C-18-0049 & C-18-0050

TRAFFIC OPERATIONS:

- TRAFFIC WILL BE CONTROLLED BY FLAGGING OPERATIONS AND SHOULDER CLOSURES ON USH 12

TRAFFIC CONTROL LEGEND

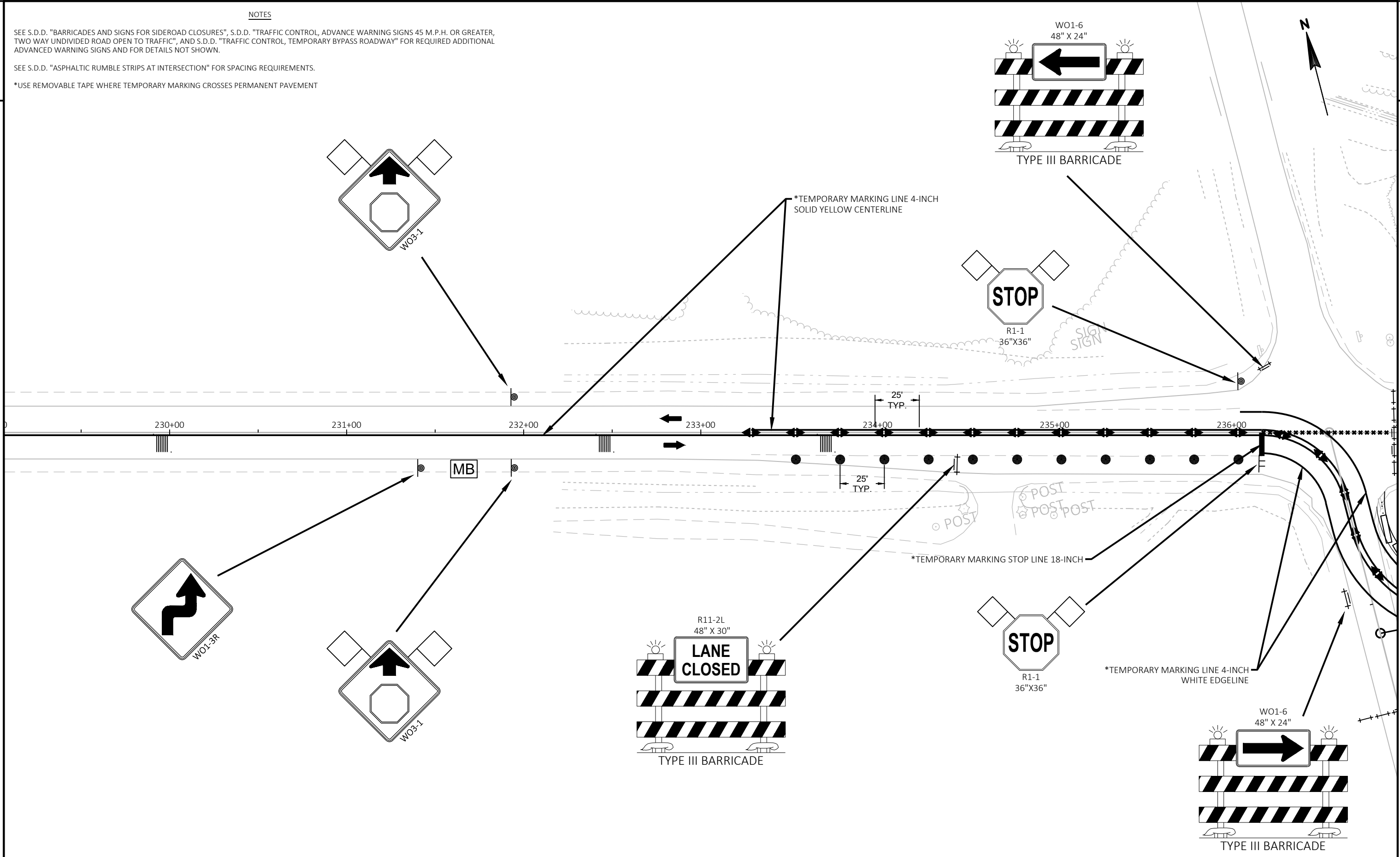
	TYPE III BARRICADE		TYPE A WARNING LIGHT (FLASHING)
	TYPE III BARRICADE WTH ATTACHED SIGN		TYPE C WARNING LIGHT (STEADY BURN)
	TRAFFIC CONTROL DRUM		CONCRETE BARRIER TEMPORARY PRECAST
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT		TEMPORARY RAISED PAVEMENT MARKER (TWO WAY REFLECTOR YELLOW)
	SIGN ON PERMANENT SUPPORT		DIRECTION OF TRAFFIC
	SIGN ON TEMPORARY SUPPORT		PORTABLE CHANGEABLE MESSAGE BOARD
	REMOVE PAVEMENT MARKING		TEMPORARY RUMBLE STRIPS INTERSECTION
	WORK AREA		TEMPORARY DELINEATOR (SINGLE DELINEATOR WHITE)
	FLAGS, 16" X 16" MIN., ORANGE (INCIDENTAL TO TRAFFIC CONTROL)		

NOTES

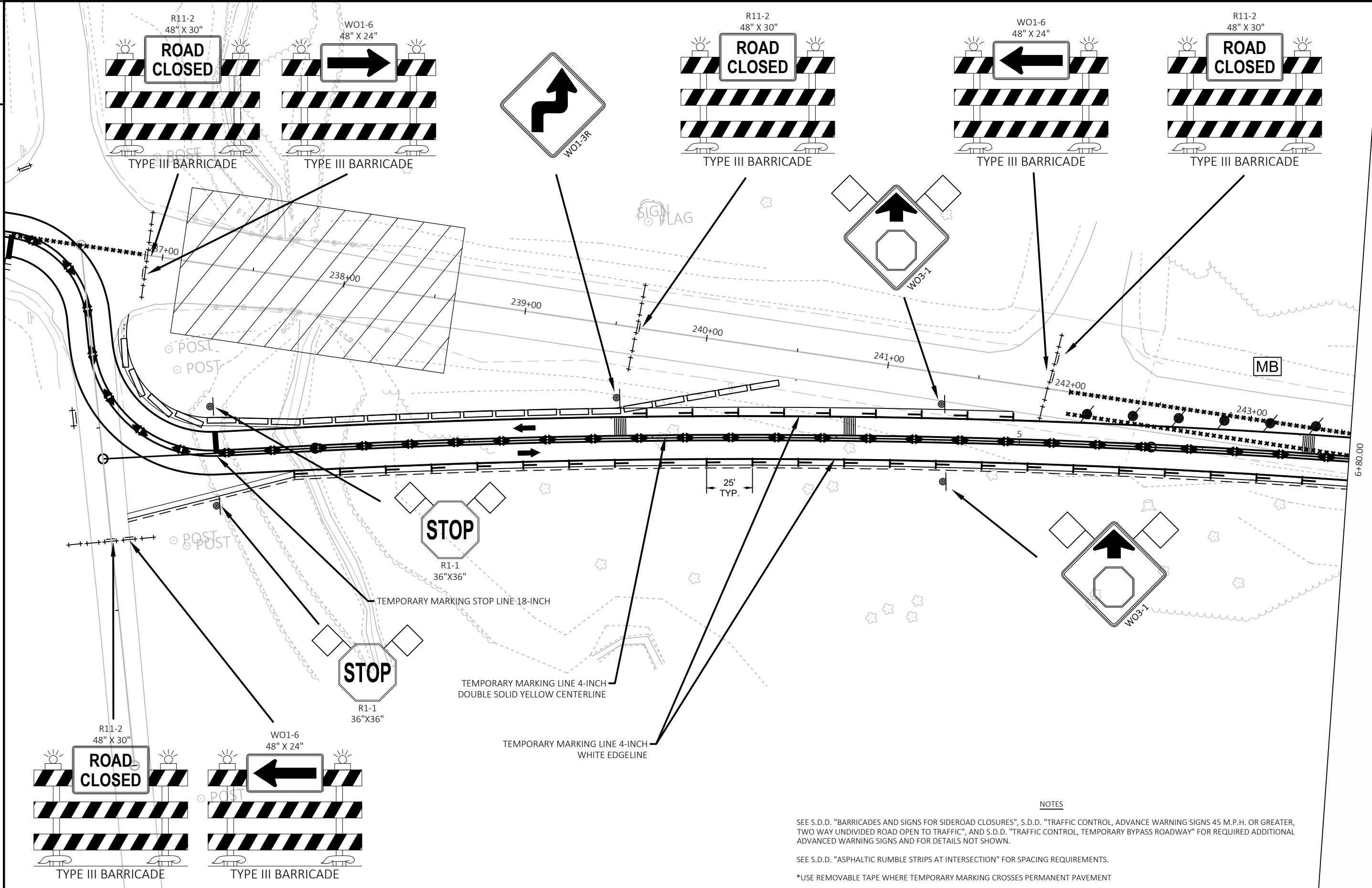
SEE S.D.D. "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES", S.D.D. "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC", AND S.D.D. "TRAFFIC CONTROL, TEMPORARY BYPASS ROADWAY" FOR REQUIRED ADDITIONAL ADVANCED WARNING SIGNS AND FOR DETAILS NOT SHOWN.

SEE S.D.D. "ASPHALTIC RUMBLE STRIPS AT INTERSECTION" FOR SPACING REQUIREMENTS.

*USE REMOVABLE TAPE WHERE TEMPORARY MARKING CROSSES PERMANENT PAVEMENT



PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	TRAFFIC CONTROL - STAGE 2	SHEET	E
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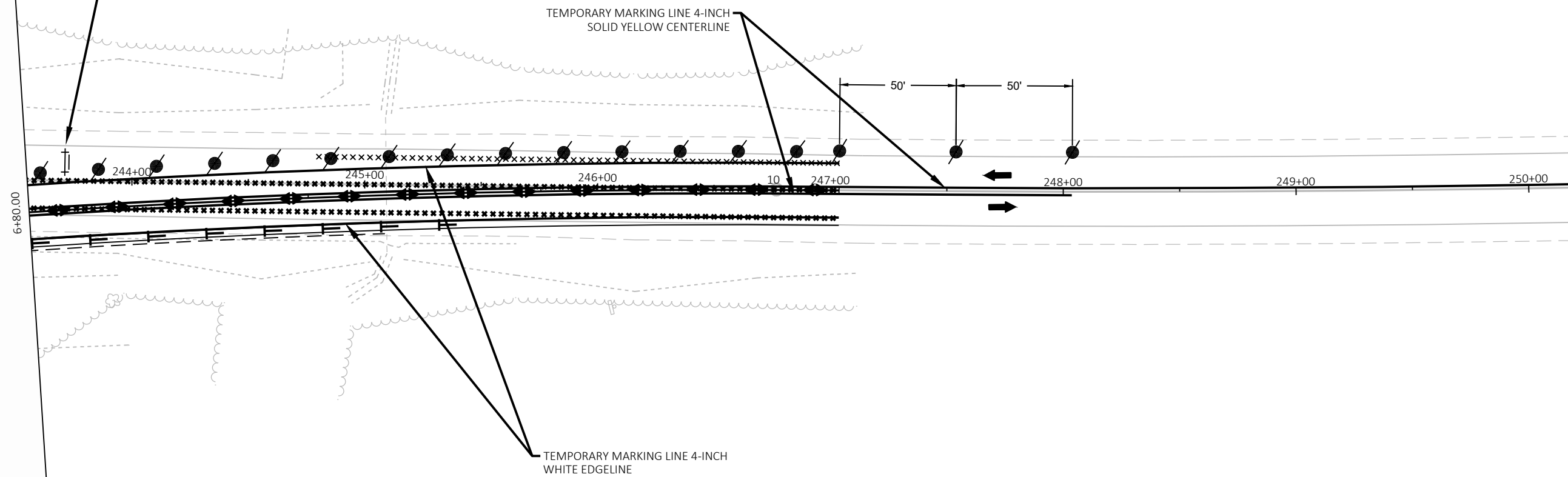
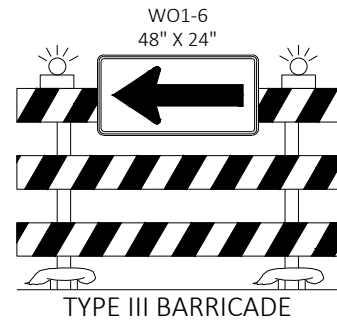
NOTES

SEE S.D.D. "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES", S.D.D. "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC", AND S.D.D. "TRAFFIC CONTROL, TEMPORARY BYPASS ROADWAY" FOR REQUIRED ADDITIONAL ADVANCED WARNING SIGNS AND FOR DETAILS NOT SHOWN.

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PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	TRAFFIC CONTROL - STAGE 2	SHEET	E
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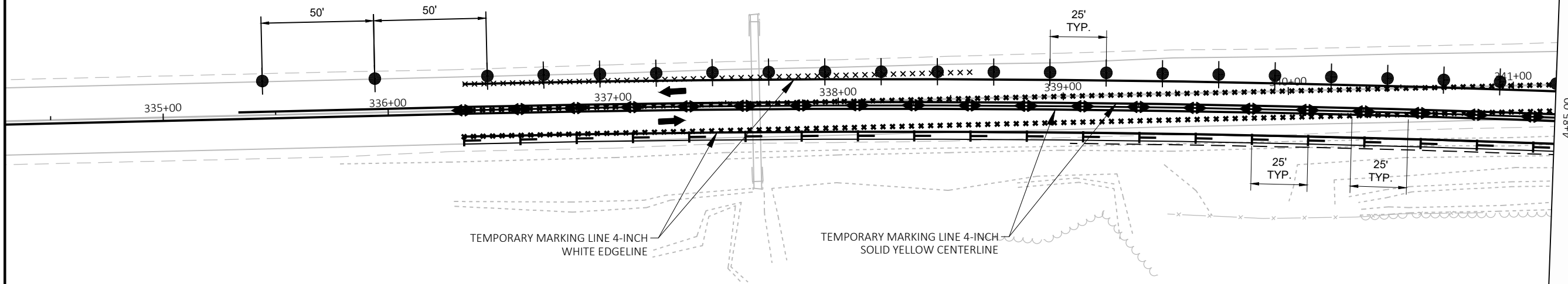
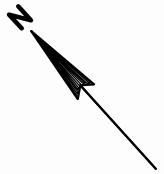
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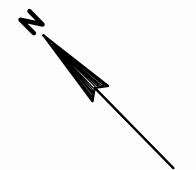
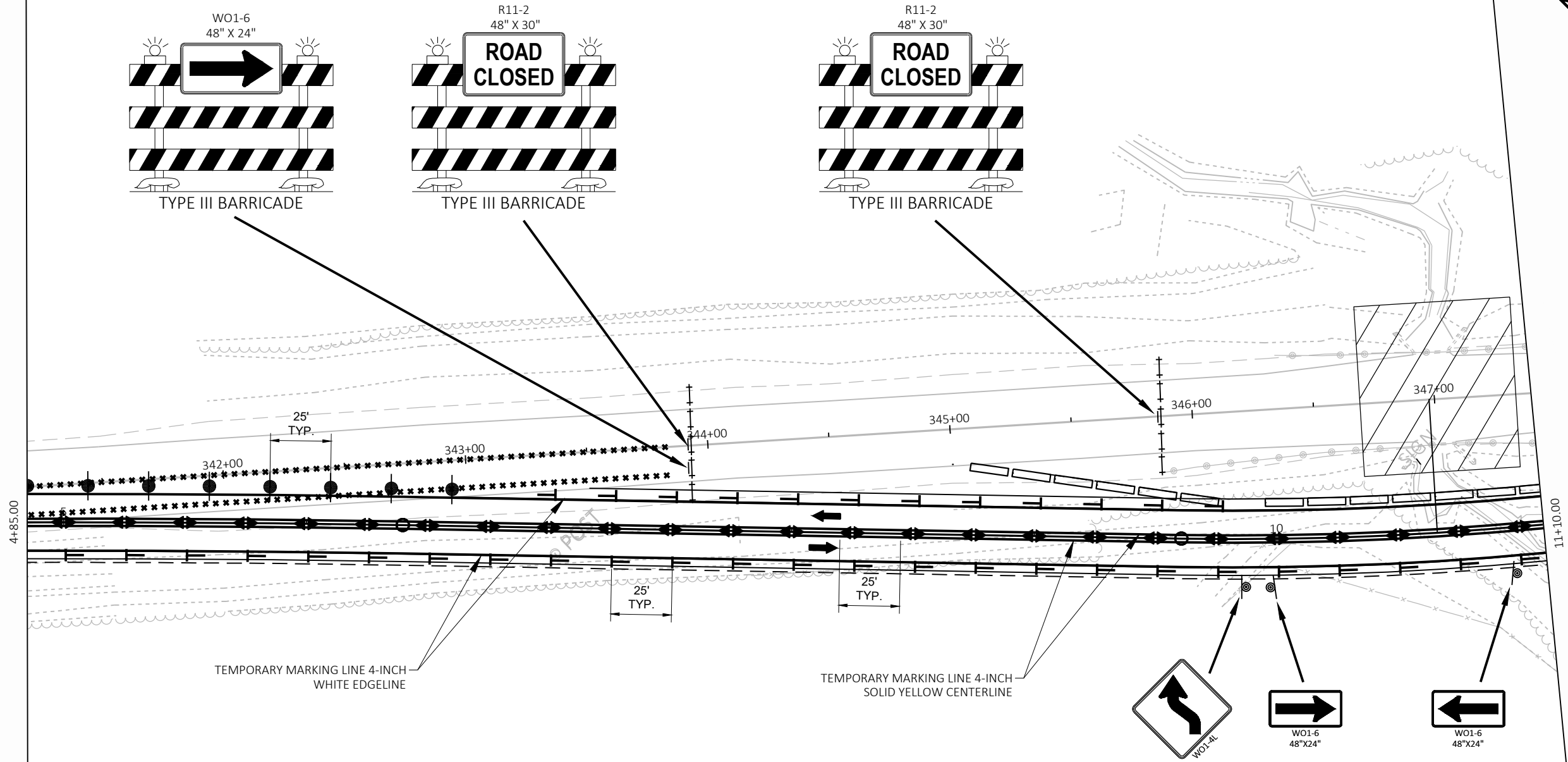
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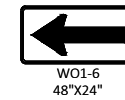
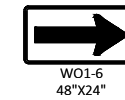
*USE REMOVABLE TAPE WHERE TEMPORARY MARKING CROSSES PERMANENT PAVEMENT





TEMPORARY MARKING LINE 4-INCH WHITE EDGELINE

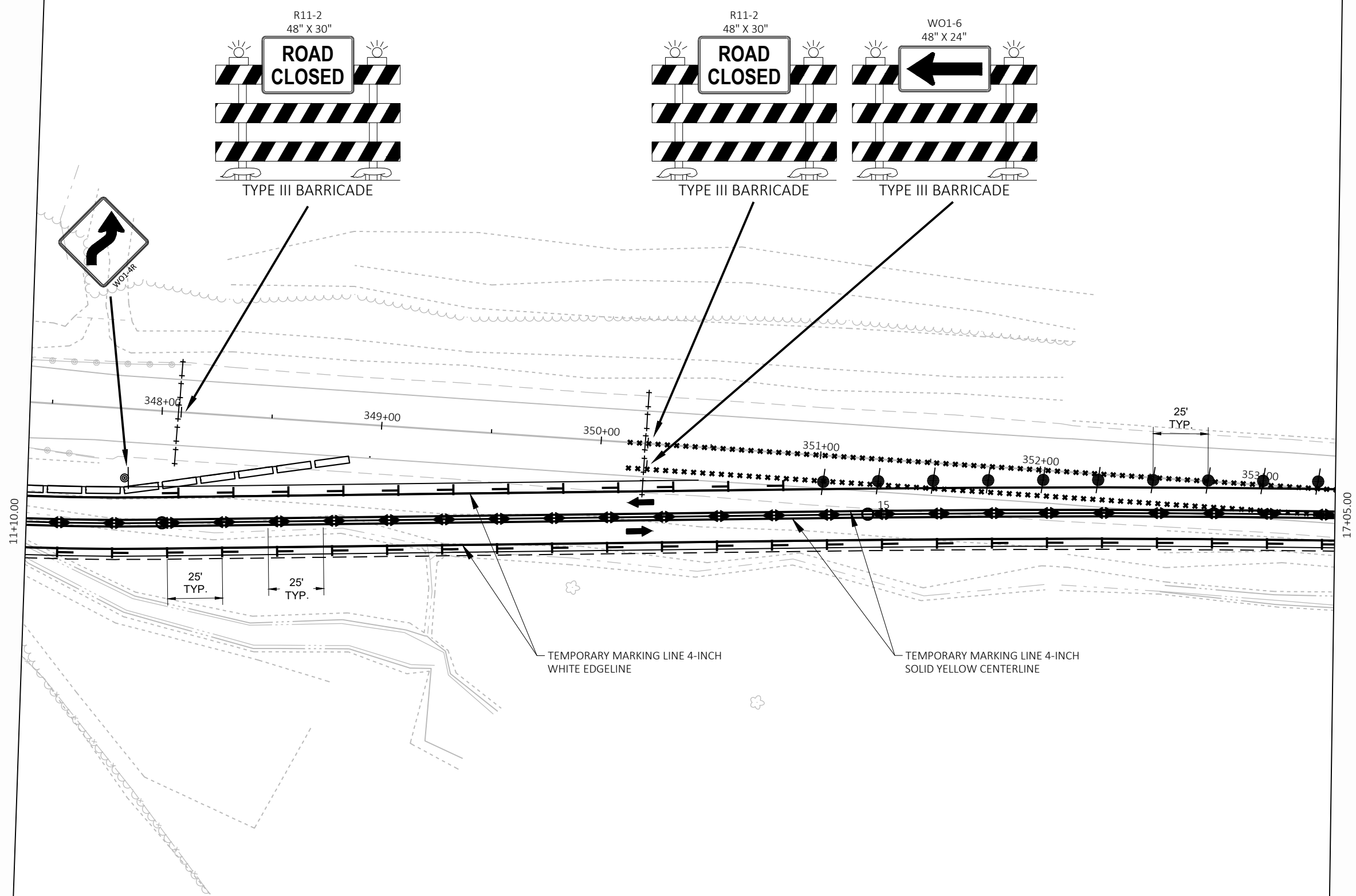
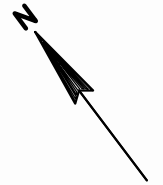
TEMPORARY MARKING LINE 4-INCH SOLID YELLOW CENTERLINE



NOTES

SEE S.D.D. "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES", S.D.D. "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC", AND S.D.D. "TRAFFIC CONTROL, TEMPORARY BYPASS ROADWAY" FOR REQUIRED ADDITIONAL ADVANCED WARNING SIGNS AND FOR DETAILS NOT SHOWN.

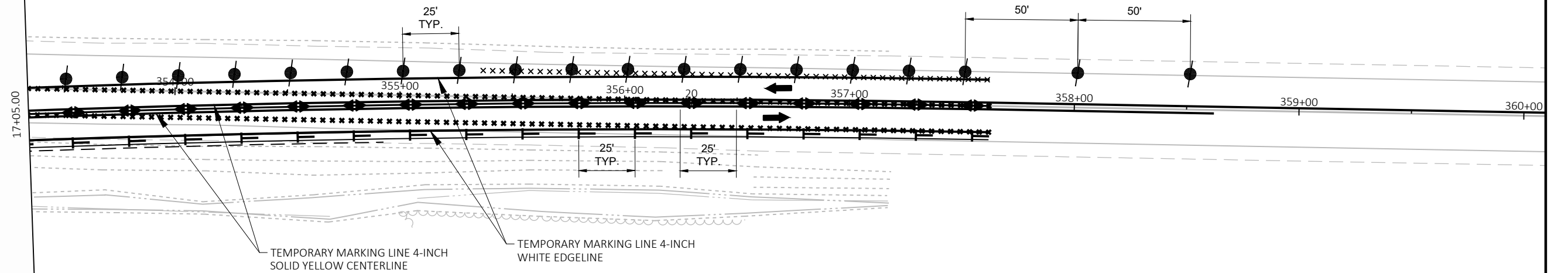
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GENERAL NOTES

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TEMPORARY MARKING LINE 4-INCH SOLID YELLOW CENTERLINE

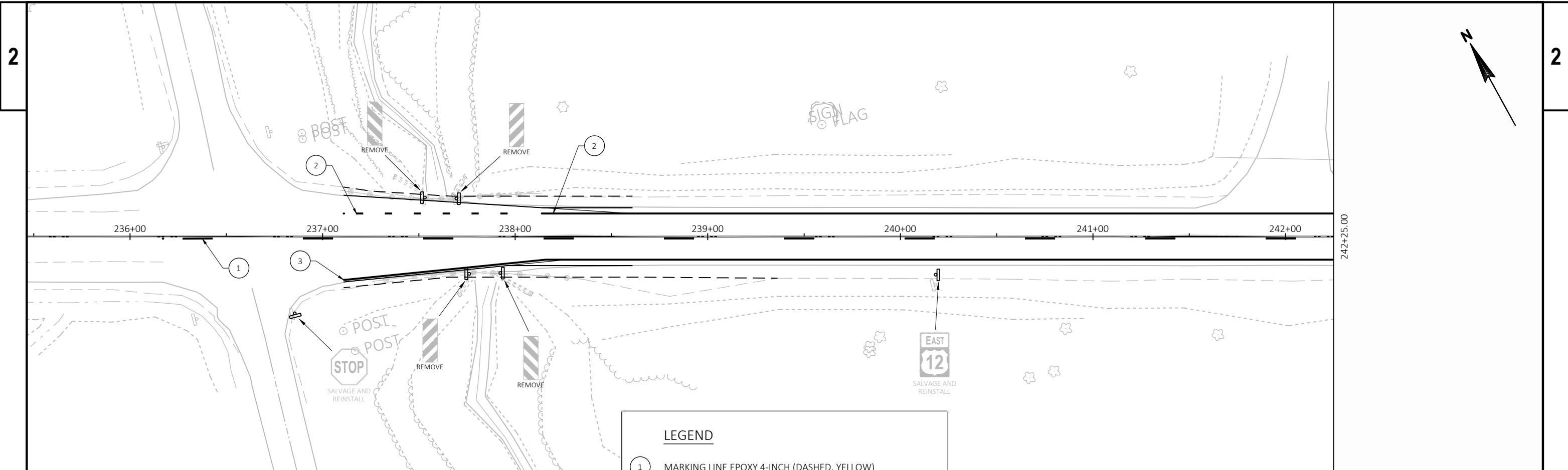
TEMPORARY MARKING LINE 4-INCH WHITE EDGELINE

GENERAL NOTES

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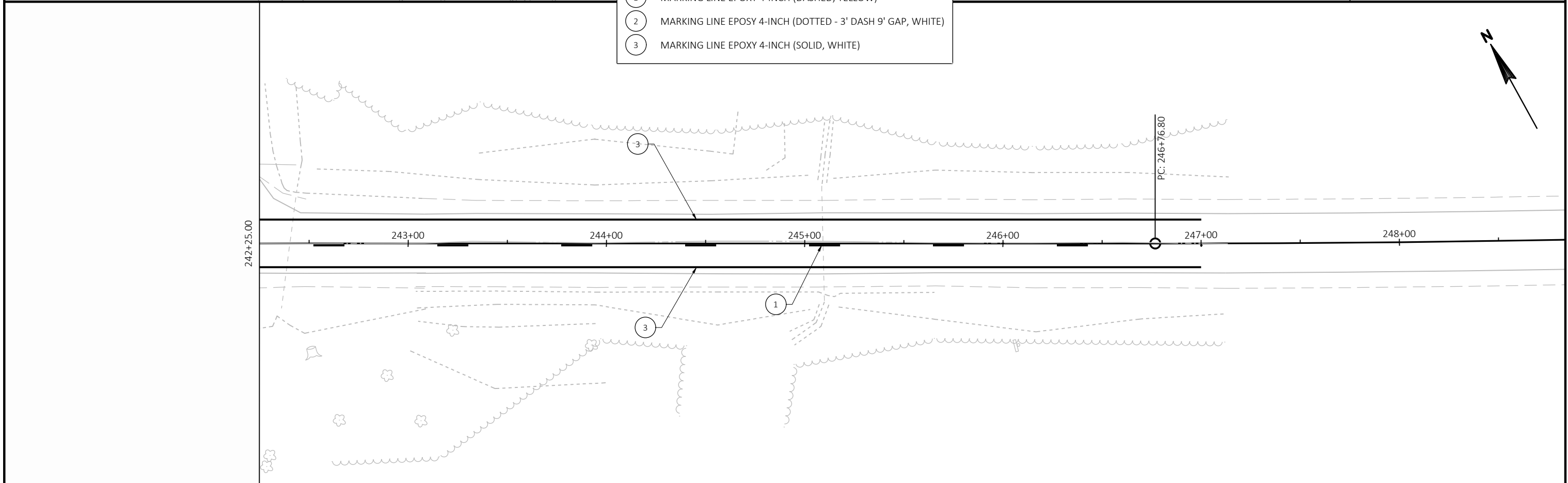
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PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	TRAFFIC CONTROL - STAGE 2	SHEET	E
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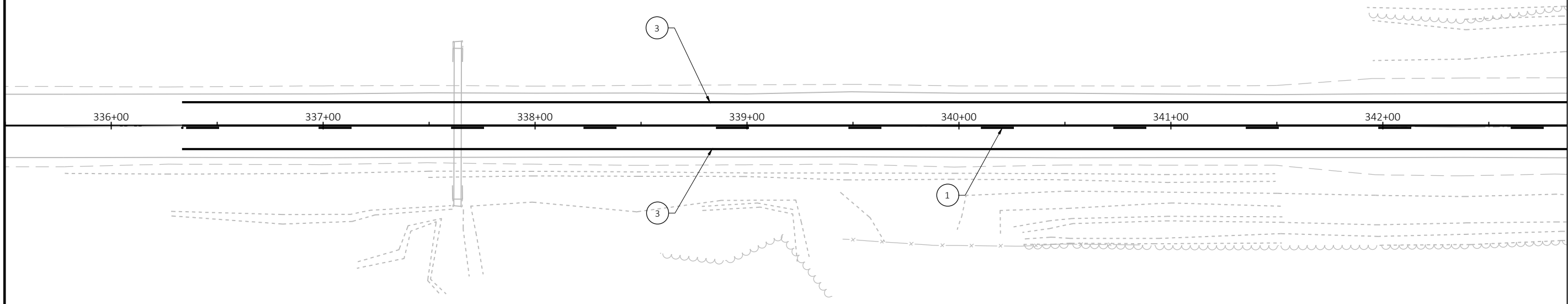


LEGEND





1	MARKING LINE EPOXY 4-INCH (DASHED, YELLOW)
2	MARKING LINE EPOXY 4-INCH (DOTTED - 3' DASH 9' GAP, WHITE)
3	MARKING LINE EPOXY 4-INCH (SOLID, WHITE)

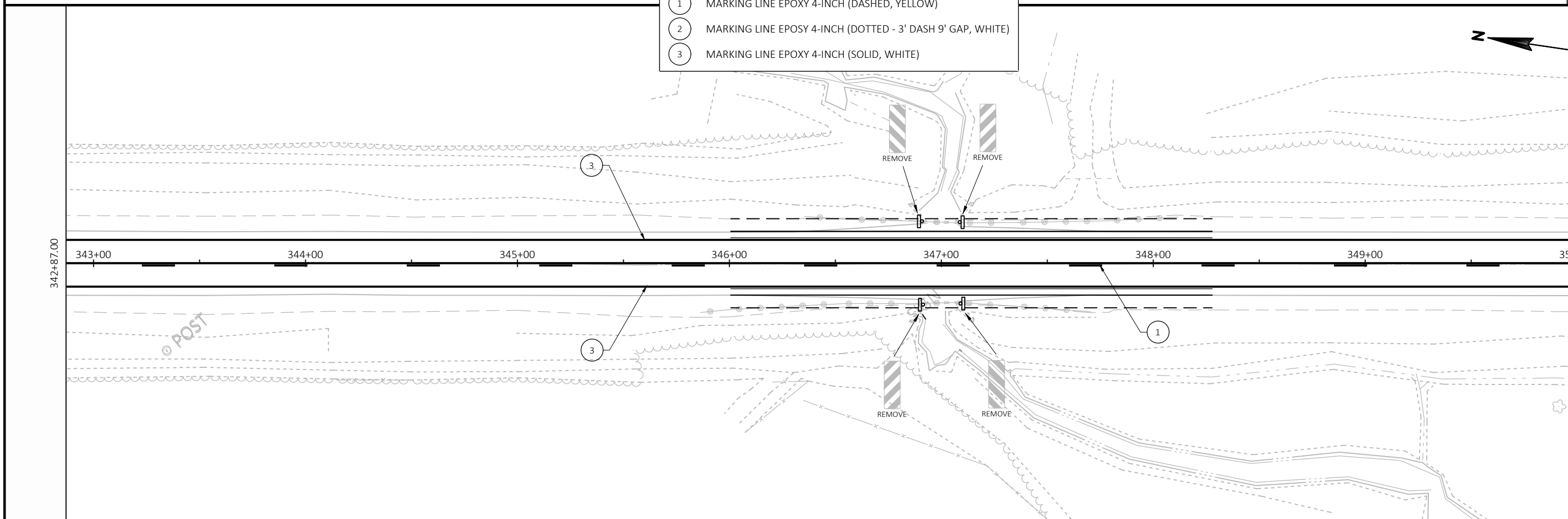


PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	USH12 SIGNING & MARKING 01	SHEET	E
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LEGEND

-  EXISTING SIGN MOUNTED ON POST
-  MARKING LINE EPOXY 4-INCH (DASHED, YELLOW)
-  MARKING LINE EPOXY 4-INCH (DOTTED - 3' DASH 9' GAP, WHITE)
-  MARKING LINE EPOXY 4-INCH (SOLID, WHITE)





350+17.00

351+00

352+00

353+00

354+00

355+00

356+00





357+00

3

3

1

LEGEND

-  EXISTING SIGN MOUNTED ON POST
-  MARKING LINE EPOXY 4-INCH (DASHED, YELLOW)
-  MARKING LINE EPOXY 4-INCH (DOTTED - 3' DASH 9' GAP, WHITE)
-  MARKING LINE EPOXY 4-INCH (SOLID, WHITE)

Estimate Of Quantities

7080-01-72

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	15.000	15.000
0004	201.0205	Grubbing	STA	15.000	15.000
0006	203.0100	Removing Small Pipe Culverts	EACH	10.000	10.000
0008	203.0200	Removing Old Structure (station) 01. 237+72	LS	1.000	1.000
0010	203.0200	Removing Old Structure (station) 02. 347+00	LS	1.000	1.000
0012	204.0100	Removing Pavement	SY	773.000	773.000
0014	204.0165	Removing Guardrail	LF	576.000	576.000
0016	204.0170	Removing Fence	LF	780.000	780.000
0018	205.0100	Excavation Common	CY	13,228.000	13,228.000
0020	206.2000	Excavation for Structures Culverts (structure) 01. C-18-0049	LS	1.000	1.000
0022	206.2000	Excavation for Structures Culverts (structure) 02. C-18-0050	LS	1.000	1.000
0024	208.0100	Borrow	CY	9,075.000	9,075.000
0026	210.2100	Backfill Structure Type B	CY	2,955.000	2,955.000
0028	213.0100	Finishing Roadway (project) 01. 7080-01-72	EACH	1.000	1.000
0030	305.0110	Base Aggregate Dense 3/4-Inch	TON	564.000	564.000
0032	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	6,446.000	6,446.000
0034	311.0115	Breaker Run	CY	294.000	294.000
0036	455.0605	Tack Coat	GAL	501.000	501.000
0038	465.0105	Asphaltic Surface	TON	449.000	449.000
0040	465.0110	Asphaltic Surface Patching	TON	32.000	32.000
0042	465.0125	Asphaltic Surface Temporary	TON	2,052.000	2,052.000
0044	504.0100	Concrete Masonry Culverts	CY	318.000	318.000
0046	505.0400	Bar Steel Reinforcement HS Structures	LB	53,110.000	53,110.000
0048	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	6,440.000	6,440.000
0050	511.1200	Temporary Shoring (structure) 01. C-18-0050	SF	390.000	390.000
0052	516.0500	Rubberized Membrane Waterproofing	SY	57.000	57.000
0054	520.2024	Culvert Pipe Temporary 24-Inch	LF	31.000	31.000
0056	520.2048	Culvert Pipe Temporary 48-Inch	LF	952.000	952.000
0058	603.8000	Concrete Barrier Temporary Precast Delivered	LF	800.000	800.000
0060	603.8125	Concrete Barrier Temporary Precast Installed	LF	800.000	800.000
0062	606.0300	Riprap Heavy	CY	103.000	103.000
0064	616.0100	Fence Woven Wire (height) 01. 4 FT	LF	780.000	780.000
0066	618.0100	Maintenance And Repair of Haul Roads (project) 01. 7080-01-72	EACH	1.000	1.000
0068	619.1000	Mobilization	EACH	1.000	1.000
0070	624.0100	Water	MGAL	100.000	100.000
0072	625.0100	Topsoil	SY	93.000	93.000
0074	625.0500	Salvaged Topsoil	SY	12,229.000	12,229.000

Estimate Of Quantities

7080-01-72

Line	Item	Item Description	Unit	Total	Qty
0076	627.0200	Mulching	SY	6,400.000	6,400.000
0078	628.1504	Silt Fence	LF	3,276.000	3,276.000
0080	628.1520	Silt Fence Maintenance	LF	6,552.000	6,552.000
0082	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0084	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0086	628.2008	Erosion Mat Urban Class I Type B	SY	5,922.000	5,922.000
0088	628.5505	Polyethylene Sheeting	SY	1,570.000	1,570.000
0090	629.0210	Fertilizer Type B	CWT	7.770	7.770
0092	630.0120	Seeding Mixture No. 20	LB	333.000	333.000
0094	630.0140	Seeding Mixture No. 40	LB	7.000	7.000
0096	630.0200	Seeding Temporary	LB	333.000	333.000
0098	630.0500	Seed Water	MGAL	200.000	200.000
0100	633.1100	Delineators Temporary	EACH	155.000	155.000
0102	638.2602	Removing Signs Type II	EACH	8.000	8.000
0104	638.3000	Removing Small Sign Supports	EACH	8.000	8.000
0106	642.5001	Field Office Type B	EACH	1.000	1.000
0108	643.0300	Traffic Control Drums	DAY	7,119.000	7,119.000
0110	643.0420	Traffic Control Barricades Type III	DAY	2,499.000	2,499.000
0112	643.0705	Traffic Control Warning Lights Type A	DAY	3,621.000	3,621.000
0114	643.0715	Traffic Control Warning Lights Type C	DAY	4,131.000	4,131.000
0116	643.0900	Traffic Control Signs	DAY	3,442.000	3,442.000
0118	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0120	643.5000	Traffic Control	EACH	1.000	1.000
0122	645.0105	Geotextile Type C	SY	659.000	659.000
0124	645.0120	Geotextile Type HR	SY	4,543.000	4,543.000
0126	646.1020	Marking Line Epoxy 4-Inch	LF	6,963.000	6,963.000
0128	646.9000	Marking Removal Line 4-Inch	LF	3,195.000	3,195.000
0130	649.0105	Temporary Marking Line Paint 4-Inch	LF	7,507.000	7,507.000
0132	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	9,300.000	9,300.000
0134	649.0760	Temporary Marking Raised Pavement Marker Type I	EACH	146.000	146.000
0136	649.0805	Temporary Marking Stop Line Paint 18-Inch	LF	12.000	12.000
0138	649.0850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	13.000	13.000
0140	650.4500	Construction Staking Subgrade	LF	2,870.000	2,870.000
0142	650.5000	Construction Staking Base	LF	2,870.000	2,870.000
0144	650.6000	Construction Staking Pipe Culverts	EACH	9.000	9.000
0146	650.6500	Construction Staking Structure Layout (structure) 01. C-18-0049	LS	1.000	1.000
0148	650.6500	Construction Staking Structure Layout (structure) 02. C-18-0050	LS	1.000	1.000
0150	650.9910	Construction Staking Supplemental Control (project) 01.	LS	1.000	1.000

Estimate Of Quantities

7080-01-72

Line	Item	Item Description	Unit	Total	Qty
		7080-01-72			
0152	650.9920	Construction Staking Slope Stakes	LF	2,870.000	2,870.000
0154	690.0150	Sawing Asphalt	LF	2,013.000	2,013.000
0156	690.0250	Sawing Concrete	LF	80.000	80.000
0158	715.0502	Incentive Strength Concrete Structures	DOL	3,150.000	3,150.000
0160	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0162	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000
0164	SPV.0060	Special 01. Salvage and Reinstall Sign	EACH	2.000	2.000
0166	SPV.0105	Special 01. Temporary Water Diversion Six Mile Creek	LS	1.000	1.000
0168	SPV.0105	Special 02. Temporary Water Diversion Nine Mile Creek	LS	1.000	1.000
0170	SPV.0105	Special 03. Temporary Rumble Strips Intersection	LS	1.000	1.000
0172	SPV.0180	Special 01. Temporary Slope Stabilization	SY	5,470.000	5,470.000

3

3

CLEARING & GRUBBING

CATEGORY	STATION TO	STATION	LOCATION	CLEARING		GRUBBING	
				201.0105	STA	201.0205	STA
0010	237+06 -	237+81	USH 12, LEFT	1		1	
0010	237+27 -	243+23	USH 12, RIGHT	6		6	
0010	340+87 -	347+45	USH 12, RIGHT	7		7	
0010	347+56 -	347+84	USH 12, LEFT	1		1	
TOTALS =				15		15	

REMOVING SMALL PIPE CULVERTS

CATEGORY	STATION	LOCATION	203.0100		REMARKS	TYPE	SIZE (IN)	LENGTH (LF)
			EACH					
0010	1+17' BP1	BYPASS	2		TEMP PIPES	CORRUGATE METAL PIPE	48	100/100
0010	238+13	USH 12	2		TEMP PIPES	CORRUGATE METAL PIPE	48	106/106
0010	1+62' BP1	BYPASS	2		TEMP PIPES	CORRUGATE METAL PIPE	48	106/106
0010	5+59' BP1	USH 12, RIGHT	1		EXTENSION ONLY	CORRUGATE METAL PIPE	24	31
0010	347+94	USH 12	2		TEMP PIPES	CORRUGATE METAL PIPE	48	164/164
0010	347+69	USH 12, LT	1		FE CULVERT	CORRUGATE METAL PIPE	18	22
TOTAL =			10					

REMOVING PAVEMENT

CATEGORY	STATION TO	STATION	LOCATION	204.0100 *	
				SY	
0010	237+11 -	237+66	USH 12	122	
0010	237+80 -	238+61	USH 12	180	
0010	346+00 -	346+92	USH 12	204	
0010	347+08 -	348+28	USH 12	267	
TOTAL =				773	

* CONCRETE PAVEMENT UNDER ASPHALTIC SURFACE

BASE AGGREGATE DENSE

CATEGORY	STATION TO	STATION	LOCATION	305.0110		305.0120		REMARKS
				TON		TON		
0010	0+10.41' BP1 -	10+28.01' BP1	BYPASS	124		1841		
0010	0+00.00' BP2 -	21+32.29' BP2	BYPASS	276		3135		
0010	237+11 -	238+61	USH 12	57		574		C-18-0049
0010	346+00 -	348+28	USH 12	107		896		C-18-0050
TOTALS =				564		6446		

DIVISION	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1)		SALVAGED/UNUSABLE PAVEMENT MATERIAL (4)	AVAILABLE MATERIAL (5)	UNEXPANDED FILL	EXPANDED FILL (13)	MASS ORDINATE +/- (14)	WASTE	208.0100 BORROW	COMMENT:
			CUT (2)	EBS EXCAVATION (3)								
STAGE 1												
1	00+00'BP1 - 10+28.01'BP1	SIX MILE BYPASS	465	0	0	465	2,563	3,204	-2,739	0	2,739	
DIVISION 2 SUBTOTAL			465	0	0	465	2,563	3,204	-2,739	0	2,739	
STAGE 1												
1	00+50'BP2 - 21+32.29'BP2	NINE MILE BYPASS	1,168	0	0	1,168	6,003	7,504	-6,336	0	6,336	
DIVISION 1 SUBTOTAL			1,168	0	0	1,168	6,003	7,504	-6,336	0	6,336	
STAGE 3												
1	237+10.85 - 357+62.13	MAINLINE	11,595	0	0	11,595	887	1,108	10,487	10,487	0	SEE NOTE 7
DIVISION 3 SUBTOTAL			11,595	0	0	11,595	887	1,108	10,487	10,487	0	
GRAND TOTAL			13,228	0	0	13,228	9,453	11,817	1,411	10,487	9,075	
TOTAL COMMON EXC			13,228									

NOTES:

- (1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
- (2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- (3) EBS EXCAVATION TO BE BACKFILLED WITH SELECT BORROW MATERIAL. NOTE: THIS IS DESIGNERS CHOICE, CAN BE BACKFILLED WITH BORROW, OR CUT AS WELL.
- (4) SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (6) EXPANDED FILL FACTOR = 1.25
- (7) No Borrow Needed

3

REMOVING GUARDRAIL

CATEGORY	STATION TO	STATION	LOCATION	204.0165 LF
0010	237+09 -	238+14	USH 12, LEFT	105
0010	237+26 -	238+35	USH 12, RIGHT	109
0010	345+86 -	347+71	USH 12, RIGHT	185
0010	346+35 -	348+12	USH 12, LEFT	177
TOTAL =				576

ASPHALTIC ITEMS

CATEGORY	STATION TO	STATION	LOCATION	TACK COAT 455.0605 GAL	ASPHALTIC SURFACE 465.0105 TON	ASPHALTIC SURFACE PATCHING 465.0110 TON	ASPHALTIC SURFACE TEMPORARY 465.0125 TON
0010	237+11 -	238+61	USH 12	58	194	-	-
0010	0+10.41'BP1 -	10+28.01'BP1	BYPASS	136	-	-	760
0010	346+00 -	348+28	USH 12	76	255	-	-
0010	0+00.00'BP2 -	21+32.29'BP2	BYPASS	231	-	-	1292
0010			S ELCO ROAD	-	-	32	-
TOTALS =				501	449	32	2052

3

REMOVING FENCE

CATEGORY	STATION TO	STATION	LOCATION	204.0170 LF
0010	340+85 -	347+52	USH 12, RIGHT	780

CONCRETE BARRIER TREMPORARY PRECAST

CATEGORY	STATION TO	STATION	LOCATION	DELIVERED 603.8000 LF	INSTALLED 603.8125 LF	REMARKS
0010	0+14.68'BP1 -	2+30.00'BP1	BYPASS, LT	400	400.00	ANCHOR BARRIER
0010	9+57.75'BP2 -	11+85.13'BP2	BYPASS, LT	400	400.00	ANCHOR BARRIER
TOTALS =				800	800	

WATER

CATEGORY	LOCATION	TASK	624.0100 MGAL	630.0500 MGAL
0010	USH 12	COMPACTION	75	-
0010	USH 12	DUST CONTROL	25	-
0010	USH 12	WATERING SEED	-	200
TOTALS =			100	200

POLYETHYLENE SHEETING

CATEGORY	PROJECT	LOCATION	628.5505 SY	REMARKS
0010	7080-01-72	C-18-0049	760	COVERING TOPSOIL STOCKPILE
0010	7080-01-72	C-18-0050	810	COVERING TOPSOIL STOCKPILE
TOTAL =			1570	

TEMPORARY CULVERT PIPES

CATEGORY	INLET			OUTLET			STAGE	LOCATION	24-INCH 520.2024	48-INCH 520.2048	REMARKS
	STATION	ELEVATION	OFFSET	STATION	ELEVATION	OFFSET			LF	LF	
0010	238+12.17	904.03	136.98	237+79.23	903.66	42.54	STG 1	BYPASS	-	100	EXISTING CHANNEL
0010	238+16.89	904.03	135.33	237+84.01	903.66	40.90	STG 1	BYPASS	-	100	EXISTING CHANNEL
0010	238+59.06	903.71	132.40	238+22.20	903.21	33.01	STG 1	BYPASS	-	106	DIVERSION CHANNEL
0010	238+63.74	903.71	130.66	238+26.89	903.21	31.28	STG 1	BYPASS	-	106	DIVERSION CHANNEL
0010	238+22.20	903.21	33.01	237+85.35	902.71	-66.37	STG 2	USH 12	-	106	DIVERSION CHANNEL
0010	238+26.89	903.21	31.28	237+90.04	902.71	-68.10	STG 2	USH 12	-	106	DIVERSION CHANNEL
0010	-	-	-	242+32.10	-	63.45	STG 1	USH 12, RIGHT	31	-	CULVERT PIPE EXTENSION
0010	348+46.71	927.88	94.31	347+63.51	927.74	-47.02	STG 1	USH 12	-	164	DIVERSION CHANNEL
0010	348+51.02	927.88	91.77	347+67.82	927.74	-49.56	STG 1	USH 12	-	164	DIVERSION CHANNEL
TOTALS =									31	952	

3

EROSION CONTROL

CATEGORY	STAGE	STATION TO	STATION	LOCATION	SILT FENCE	SILT FENCE
					628.1504	628.1520
0010	1	0+28'BP1 -	7+81'BP1	BYPASS, RT	804	1608
0010	1	0+28'BP1 -	3+06'BP1	BYPASS, LT	346	692
0010	1	0+54'BP2 -	2+75'BP2	BYPASS, RT	264	528
0010	1	3+98'BP2 -	18+34'BP2	BYPASS, RT	1492	2984
0010	3	237+32	237+49	USH 12, LT	73	146
0010	3	238+27	238+83	USH 12, RT	70	140
0010	3	345+99	346+83	USH 12, LT	86	172
0010	3	347+18	348+29	USH 12, LT	141	282
TOTALS =					3276	6552

FENCE WOVEN WIRE (HEIGHT) 4 FT

CATEGORY	STATION TO	STATION	LOCATION	616.0100
				LF
0010	340+85 -	347+52	USH 12, RIGHT	780

3

REMOVING SIGNS

CATEGORY	STA	SIGN #	SIGN CODE	LOCATION	DESCRIPTION	REMOVING	REMOVING SMALL
						SIGNS TYPE II	SIGN SUPPORTS
						638.2602	638.3000
						EACH	EACH
0010	237+52	1R-01	W5-52L	USH 12, LT	BRIDGE HASH MARKS	1	1
0010	237+70	1R-02	W5-52R	USH 12, LT	BRIDGE HASH MARKS	1	1
0010	237+75	1R-03	W5-52L	USH 12, RT	BRIDGE HASH MARKS	1	1
0010	237+93	1R-04	W5-52R	USH 12, RT	BRIDGE HASH MARKS	1	1
0010	346+90	2R-01	W5-52L	USH 12, LT	BRIDGE HASH MARKS	1	1
0010	346+91	2R-02	W5-52R	USH 12, RT	BRIDGE HASH MARKS	1	1
0010	347+09	2R-03	W5-52R	USH 12, LT	BRIDGE HASH MARKS	1	1
0010	347+10	2R-04	W5-52L	USH 12, RT	BRIDGE HASH MARKS	1	1
TOTALS =						8	8

EROSION CONTROL MOBILIZATION

CATEGORY	PROJECT	MOBILIZATION	MOBILIZATION
		EROSION CONTROL	EMERGENCY
		628.1905	EROSION CONTROL
		EACH	628.1910
		EACH	EACH
0010	7080-01-72	4	2

GEOTEXTILE TYPE HR

CATEGORY	STATION TO	STATION	LOCATION	645.0120	REMARKS
				SY	
0010	0+60'BP1 -	1+59'BP1	BYPASS	883	WETLAND PROTECTION
0010	8+77'BP2 -	17+22'BP2	BYPASS	3403	WETLAND PROTECTION
TOTAL =				4286.00	

TRAFFIC CONTROL

CATEGORY	STAGE	DRUMS	BARRICADES	WARNING LIGHTS		SIGNS	SIGNS
		643.0300	TYPE III	TYPE A	TYPE C	643.0900	PCMS
		DAY	DAY	DAY	DAY	DAY	DAY
0010	STAGE 1	1482	-	-	-	235	-
0010	STAGE 2	4692	2499	3621	4131	2907	-
0010	STAGE 3	945	-	-	-	150	-
0010	UNDISTRIBUTED	-	-	-	-	150	14
TOTALS =		7119	2499	3621	4131	3442	14

PAVEMENT MARKING

CATEGORY	LOCATION	MARKING LINE		MARKING REMOVAL	
		WHITE	YELLOW	WHITE	YELLOW
		646.1020		646.9000	
		LF	LF	LF	LF
0010	USH 12	6160	803	2675	520
TOTALS =		6963		3195	

RESTORATION

CATEGORY	STATION TO	STATION	LOCATION	TOPSOIL	SALVAGED	MULCHING	EROSION MAT	FERTILIZER	SEEDING	SEEDING	SEEDING	REMARKS
				625.0100	TOPSOIL		URBAN CLASS I	TYPE B	MIXTURE NO. 20	MIXTURE NO. 40	TEMPORARY	
				SY	625.0500	SY	TYPE B	629.0210	630.0120	630.0140	630.0200	
							628.2008	CWT	LB	LB	LB	
0010	236+83	238+92	USH 12, RT	-	1839	-	1839	1.16	50	-	50	
0010	237+11	238+61	USH 12, LT	-	1077	-	1077	0.68	29	7	29	
0010	238+63	247+04	USH 12, RT	-	2897	2897	-	1.83	78	-	78	
0010	336+33	339+02	USH 12, RT	-	360	360	-	0.23	10	-	10	
0010	340+42	345+10	USH 12, RT	-	1244	1244	-	0.78	34	-	34	
0010	340+89	345+10	USH 12, RT	-	333	-	333	0.21	9	-	9	DITCH BOTTOM
0010	341+09	347+43	USH 12, RT	-	1899	1899	-	1.20	51	-	51	
0010	345+10	357+62	USH 12, RT	-	1924	-	1924	1.21	52	-	52	
0010	346+00	348+28	USH 12, LT	93	656	-	749	0.47	20	-	20	
TOTALS =				93	12229	6400	5922	7.77	333	7	333	

TEMPORARY DELINEATORS & PAVEMENT MARKINGS

CATEGORY	STAGE	LOCATION	DELINEATOR	TEMPORARY	TEMPORARY	TEMPORARY	TEMPORARY	TEMPORARY	TEMPORARY	TEMPORARY
			TEMPORARY	MARKING LINE	MARKING LINE	MARKING RAISED	MARKING	MARKING	MARKING	
			633.1100	PAINT 4-INCH	REMOVABLE TAPE	PAVEMENT MARKER	STOP LINE	STOP LINE	STOP LINE	STOP LINE
			WHITE	4-INCH	4-INCH	TYPE I	PAINT 18-INCH	PAINT 18-INCH	PAINT 18-INCH	18-INCH
			EACH	649.0105	649.0150	YELLOW	649.0805	649.0805	649.0805	649.0850
			LF	WHITE	YELLOW	WHITE	YELLOW	WHITE	YELLOW	WHITE
			LF	LF	LF	LF	LF	LF	LF	LF
0010	STAGE 2	SIX MILE BYPASS	42	1406	1328	900	3043	59	12	13
0010	STAGE 2	NINE MILE BYPASS	113	2483	2290	1782	3575	87	-	-
TOTALS =			155	7507	9300	146	12	13		

CONSTRUCTION STAKING

CATEGORY	STATION TO	STATION	LOCATION	SUBGRADE	BASE	SLOPE STAKES	PIPE CULVERTS
				650.4500	650.5000	650.9920	650.6000
				LF	LF	LF	EACH
0010	0+09' BP1	- 8+12' BP1	TB1	803	803	803	4.00
0010	1+59' BP2	- 18+48' BP2	TB2	1689	1689	1689	-
0010	237+11	- 238+61	USH 12	150	150	150	3.00
0010	346+00	- 348+28	USH 12	228	228	228	2.00
TOTALS =				2870	2870	2870	9

SAWING ASPHALT

CATEGORY	STATION TO	STATION	LOCATION	690.0150 LF
0010			S ELCO RD, RIGHT	115
0010	241+71 -	247+04	USH 12, RIGHT	539
0010	336+33 -	343+52	USH 12, RIGHT	725
0010	350+43 -	357+62	USH 12, RIGHT	725
0010	*237+11		USH 12, RIGHT	14
0010	*237+11		USH 12, LEFT	12
0010	*238+61		USH 12, RIGHT	5
0010	*238+61		USH 12, LEFT	5
0010	*346+00		USH 12, RIGHT	5
0010	*346+00		USH 12, LEFT	5
0010	*348+28		USH 12, RIGHT	5
0010	*348+28		USH 12, LEFT	5
0010	241+71 -	247+04	USH 12, RIGHT	533
0010	336+33 -	343+52	USH 12, RIGHT	719
0010	350+43	357+62	USH 12, RIGHT	719
TOTAL =				2013

*THE EXISTING PAVEMENT IS UNDERLAIN WITH 10 FOOT DRIVING LANES
THUS THE PORTION BETWEEN THESE SAW CUTS WILL BE PAID AS "SAWING
CONCRETE". REFER TO STANDARD SPECIFICATION 690.4 NOTE (3)

SALVAGE AND REINSTALL SIGN

CATEGORY	STATION	LOCATION	SPV.0060.01 EACH	REMARKS
0010	236+85	USH 12, RIGHT	1	STOP SIGN
0010	240+18	USH 12, RIGHT	1	USH 12 EAST ROUTE SIGN
TOTAL =			2	

SAWING CONCRETE

CATEGORY	STATION	LOCATION	690.0250 LF
0010	237+11	USH 12	20
0010	238+61	USH 12	20
0010	346+00	USH 12	20
0010	348+28	USH 12	20
TOTAL =			80

TEMPORARY WATER DIVERSION

CATEGORY	LOCATION	SIX MILE CREEK	NINE MILE CREEK
		SPV.0105.01 LS	SPV.0105.02 LS
0010	USH 12	1	1
TOTALS =		1	1

TEMPORARY RUMBLE STRIPS INTERSECTION

CATEGORY	STATION TO	STATION	LOCATION	SPV.0105.03 LS
0010	230+00 -	243+37	USH 12	1

TEMPORARY SLOPE STABILIZATION

CATEGORY	STATION TO	STATION	STAGE	LOCATION	SPV.0180.01 SY
0010	0+31 BP1	3+23 BP1	STAGE 1	SIX MILE BYPASS, LEFT	682
0010	0+37 BP1 -	7+75 BP1	STAGE 1	SIX MILE BYPASS, RIGHT	946
0010	0+60 BP2 -	2+68 BP2	STAGE 1	NINE MILE BYPASS, RIGHT	265
0010	3+99 BP2 -	18+30 BP2	STAGE 1	NINE MILE BYPASS, RIGHT	3439
0010	9+74 BP2 -	11+72 BP2	STAGE 1	NINE MILE BYPASS, LEFT	138
TOTAL =					5470

TRANSPORTATION PROJECT PLAT NO: 7080-01-22-4.01

PART OF THE SE 1/4 OF THE SE 1/4, AND PART OF THE SW 1/4 OF THE SE 1/4, SECTION 29, TOWNSHIP 27 NORTH, RANGE 8 WEST, TOWN OF WASHINGTON, EAU CLAIRE COUNTY, WISCONSIN.

RELOCATION ORDER OF USH 12 EAU CLAIRE - FALL CREEK, ELCO ROAD - OAK KNOLL ROAD, EAU CLAIRE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES
			NEW	EXISTING	TOTAL	
1	EAU CLAIRE ELECTRIC CO-OP	FEE, TLE	0.07	0.35	0.42	0.29
2	THALER OIL COMPANY INC	FEE	0.10	0.16	0.26	-

1173806
 KATHRYN A. CHRISTENSON
 EAU CLAIRE COUNTY WI
 REGISTER OF DEEDS
 RECORDED ON
 04/25/2019 09:02 AM
 REC FEE:
 TRANS FEE:
 EXEMPT #:
 PAGES: 1
 This document was electronically recorded and returned to submitter

RESERVED FOR REGISTER OF DEEDS
 PROJECT NUMBER 7080-01-22-4.01

ROAD NAME	BASIS OF EXISTING R/W	YEAR
USH 12	R/W PLAT T06-4(35) AUX.3.1	1955
USH 12	R/W PLAT 395-G	1932
N ELCO RD	WI STATUTE 82.31(2)	
S ELCO RD	WI STATUTE 82.31(2)	

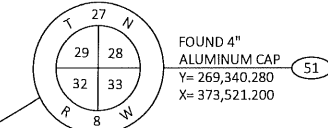
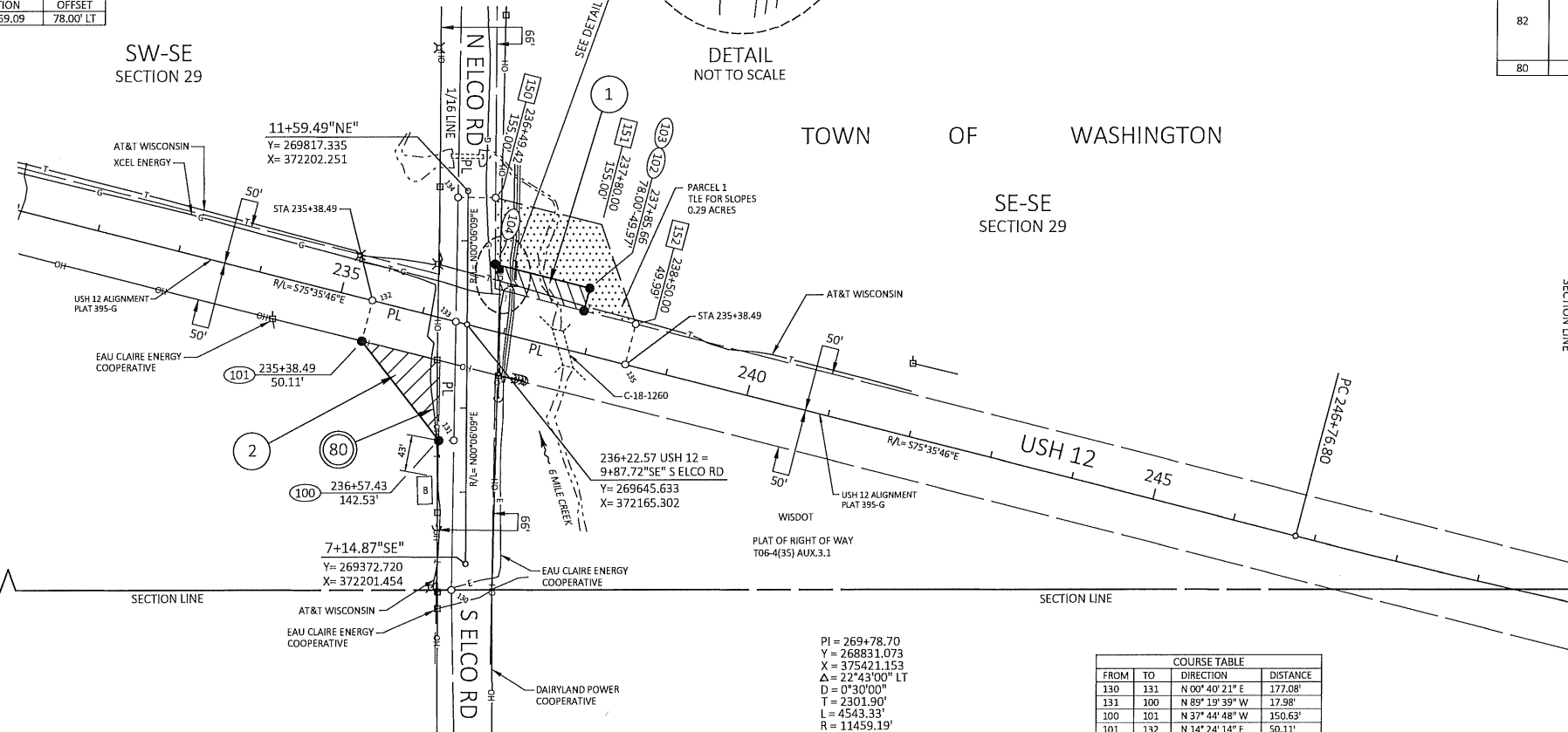
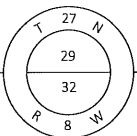
UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER(S)	INTEREST REQUIRED
80	AT&T WISCONSIN	RELEASE OF RIGHTS
81	DAIRYLAND POWER COOPERATIVE	RELEASE OF RIGHTS
82	EAU CLAIRE ENERGY COOPERATIVE	RELEASE OF RIGHTS

UTILITY EASEMENT TABLE			
UTILITY NUMBER	PARCEL	RECORDING INFORMATION	NAME
80	1	DOC 155566	WISCONSIN TELEPHONE COMPANY
81	1	DOC 476261	DAIRYLAND POWER COOPERATIVE
81	1	DOC 271982	NORTHERN STATES POWER COMPANY
82	2	DOC 2980854	EAU CLAIRE ENERGY COOPERATIVE
		DOC 586819	
		DOC 530536	
		DOC 202548	
80	2	NO RECORD OF EASEMENT	

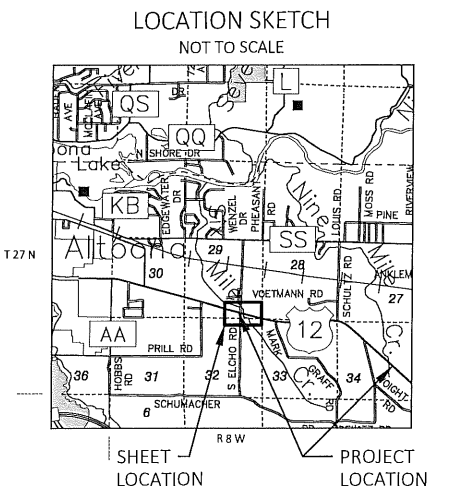
LOCATION TABLE		
POINT	STATION	OFFSET
104	236+69.09	78.00' LT

COURSE TABLE - SECTION LINE			
FROM	TO	DIRECTION	DISTANCE
50	130	S 89° 55' 46" E	1336.47'
130	51	S 89° 55' 46" E	1336.47'
51	50	S 89° 55' 46" E	2672.94'

FOUND 4" BRASS CAP IN CONCRETE
 Y = 269,343.570
 X = 370,848.260



FOUND 4" ALUMINUM CAP
 Y = 269,340.280
 X = 373,521.200



CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
BUILDING	B	EASEMENT	
CENTERLINE	C/L	RIGHT	RT
CERTIFIED SURVEY MAP	CSM	RIGHT OF WAY	R/W
CONCRETE	CONC	SECTION	SEC
COUNTY	CO	SEPTIC VENT	SEPV
COUNTY TRUNK HIGHWAY	CTH	SQUARE FOOT	SF
DISTANCE	DIST	STATE TRUNK HIGHWAY	STH
CORNER	COR	STATION	STA
DOCUMENT NUMBER	DOC	TELEPHONE PEDESTAL	TP
EASEMENT	EASE	TEMPORARY LIMITED	TLE
EXISTING	EX	EASEMENT	
FRACTIONAL	FRAC	TRANSPORTATION PROJECT	TPP
GAS VALVE	GV	PLAT	
GRID NORTH	GN	UNITED STATES HIGHWAY	USH
HIGHWAY EASEMENT	HE	VOLUME	V
IDENTIFICATION	ID		
LAND CONTRACT	LC		
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

TOWN OF WASHINGTON

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL	○	R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SIXTEENTH LINE	---	NON-MONUMENTED R/W POINT	○
NEW REFERENCE LINE	---	NEW R/W LINE	---	FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
EXISTING R/W OR HE LINE	---	PROPERTY LINE	---	OFF-PREMISE SIGN	IP
SIXTEENTH CORNER MONUMENT	□	GEODETIC SURVEY MONUMENT	□		
PROPERTY LINE	---	SIXTEENTH CORNER MONUMENT	□		
LOT, TIE & OTHER MINOR LINES	---	SIGN	IP		
SLOPE INTERCEPT	---	COMPENSABLE	---	NON-COMPENSABLE	---
CORPORATE LIMITS	---	ELECTRIC POLE	---	TELEPHONE POLE	---
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)	---		
NEW R/W (FEE OR HE) (MATCHING VARIES BY OWNER)	---	ACCESS RESTRICTED BY ACQUISITION	---	NO ACCESS (BY STATUTORY AUTHORITY)	---
TEMPORARY LIMITED EASEMENT AREA	---	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	---	NO ACCESS (NEW HIGHWAY)	---
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	PARCEL NUMBER	25	UTILITY NUMBER	40
TRANSMISSION STRUCTURES	---	BUILDING TO BE REMOVED	---	BRIDGE	---
PARALLEL OFFSETS	---				

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	Δ
CENTRAL ANGLE	Δ / DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

WATER	---	NON-COMPENSABLE	---	COMPENSABLE	---
GAS	---	TELEPHONE POLE	---	TELEPHONE PEDESTAL	---
TELEPHONE	---	OVERHEAD TRANSMISSION LINES	---	FIBER OPTIC	---
CABLE TELEVISION	---	SANITARY SEWER	---	STORM SEWER	---
POWER POLE	---	NON-COMPENSABLE	---	COMPENSABLE	---

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1/2" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

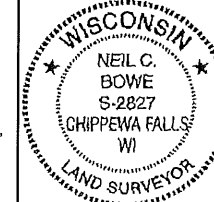
DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NW REGION.

PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS & INTERESTS REQUIRED.



SIGNATURE: *Neil C. Bowe* DATE: 4/22/19
 PRINT NAME: NEIL C. BOWE
 REGISTRATION NUMBER: 5-2827
 THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION.
 SIGNATURE: *Debra B. Stensland* DATE: 4/22/19
 PRINT NAME: DEBRA B. STENSLAND

TRANSPORTATION PROJECT PLAT NO: 7080-01-22-4.02

PART OF THE SW 1/4 OF THE NE 1/4, AND PART OF THE NW 1/4 OF THE SE 1/4, SECTION 34, TOWNSHIP 27 NORTH, RANGE 8 WEST, TOWN OF WASHINGTON, EAU CLAIRE COUNTY, WISCONSIN.

RELOCATION ORDER OF USH 12 EAU CLAIRE - FALL CREEK, ELCO ROAD - OAK KNOLL ROAD, EAU CLAIRE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WCSRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLE)S ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

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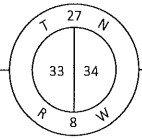
FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN THE NW REGION.

PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS & INTERESTS REQUIRED.

CONVENTIONAL UTILITY SYMBOLS

WATER	—W—
GAS	—G—
TELEPHONE	—T—
OVERHEAD TRANSMISSION LINES	—OH—
ELECTRIC	—E—
CABLE TELEVISION	—TV—
FIBER OPTIC	—FO—
SANITARY SEWER	—SAN—
STORM SEWER	—SS—
NON COMPENSABLE	—N—
COMPENSABLE	—C—
POWER POLE	—P—
TELEPHONE POLE	—TP—
TELEPHONE PEDESTAL	—TPD—
ELECTRIC TOWER	—ET—

FOUND 4" BRASS CAP IN CONCRETE
Y= 266,624.550
X= 378,847.590



CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
BUILDING	B	RIGHT	RT
CENTERLINE	C/L	RIGHT OF WAY	R/W
CERTIFIED SURVEY MAP	CSM	SECTION	SEC
CONCRETE	CONC	SEPTIC VENT	SEP
COUNTY	CO	SQUARE FEET	SF
COUNTY TRUNK HIGHWAY	CTH	STATE TRUNK HIGHWAY	STH
DISTANCE	DIST	STATION	STA
CORNER	COR	TELEPHONE PEDESTAL	TP
DOCUMENT NUMBER	DOC	TEMPORARY LIMITED EASEMENT	TLE
EASEMENT	EASE	TRANSPORTATION PROJECT	TPP
EXISTING	EX	UNITED STATES HIGHWAY	USH
FRACTIONAL	FRAC	VOLUME	V
GAS VALVE	GV		
GRID NORTH	GN		
HIGHWAY EASEMENT	HE		
IDENTIFICATION	ID		
LAND CONTRACT	LC		
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY NUMBER	NGS NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL SYMBOLS

SECTION LINE	---
QUARTER LINE	---
SIXTEENTH LINE	---
NEW REFERENCE LINE	---
NEW R/W LINE	---
EXISTING R/W OR HE LINE	---
PROPERTY LINE	---
LOT, TIE & OTHER MINOR LINES	---
SLOPE INTERCEPT	---
CORPORATE LIMITS	---
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---
NEW R/W (FREE OR HE) (MATCHING VARS BY OWNER)	---
TEMPORARY LIMITED EASEMENT AREA	---
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---
TRANSMISSION STRUCTURES	---
BUILDING TO BE REMOVED	---
BRIDGE	---
R/W MONUMENT (TIE SET)	●
NON-MONUMENTED R/W POINT	○
FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
SECTION CORNER MONUMENT	□
GEODETIC SURVEY MONUMENT	⊙
SIXTEENTH CORNER MONUMENT	⊙
SIGN	---
ELECTRIC POLE	---
TELEPHONE POLE	---
PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)	---
ACCESS RESTRICTED BY ACQUISITION	---
NO ACCESS (BY STATUTORY AUTHORITY)	---
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	---
NO ACCESS (NEW HIGHWAY)	---
PARCEL NUMBER	25
UTILITY NUMBER	40
PARALLEL OFFSETS	---

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER (S)	INTEREST REQUIRED
82	EAU CLAIRE ENERGY COOPERATIVE	RELEASE OF RIGHTS
83	CENTURYLINK	RELEASE OF RIGHTS

UTILITY EASEMENT TABLE				
UTILITY NUMBER	PARCEL	RECORDING INFORMATION	NAME	
82	6	DOC 401900	EAU CLAIRE ELECTRIC COOPERATIVE	
83	4	DOC 795652	CENTURYTEL OF CENTRAL WISCONSIN, LLC	
		DOC 375343	GENERAL TELEPHONE COMPANY OF WISCONSIN	
		DOC 155245	WISCONSIN TELEPHONE COMPANY	

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES
			NEW	EXISTING	TOTAL	
4	ANITA D WARKE	TLE	-	-	-	0.32
5	ARDIS WELKE	TLE	-	-	-	0.15
6	EDWARD S & CAROL M NOGOSEK	TLE	-	-	-	0.62

ROAD NAME	BASIS OF EXISTING R/W	YEAR
USH 12	R/W PLAT 395-G	1932

COURSE TABLE			
FROM	TO	DIRECTION	DISTANCE
232	202	N 41° 23' 19" W	308.53'
202	203	S 38° 16' 12" W	57.85'
203	204	S 00° 02' 39" E	55.80'
204	205	N 49° 11' 49" W	490.22'
205	250	S 40° 47' 47" W	120.00'
250	253	S 49° 11' 49" E	1321.49'
253	200	N 40° 48' 11" E	170.00'
200	201	N 49° 11' 49" W	505.72'
201	232	N 41° 23' 19" W	59.50'

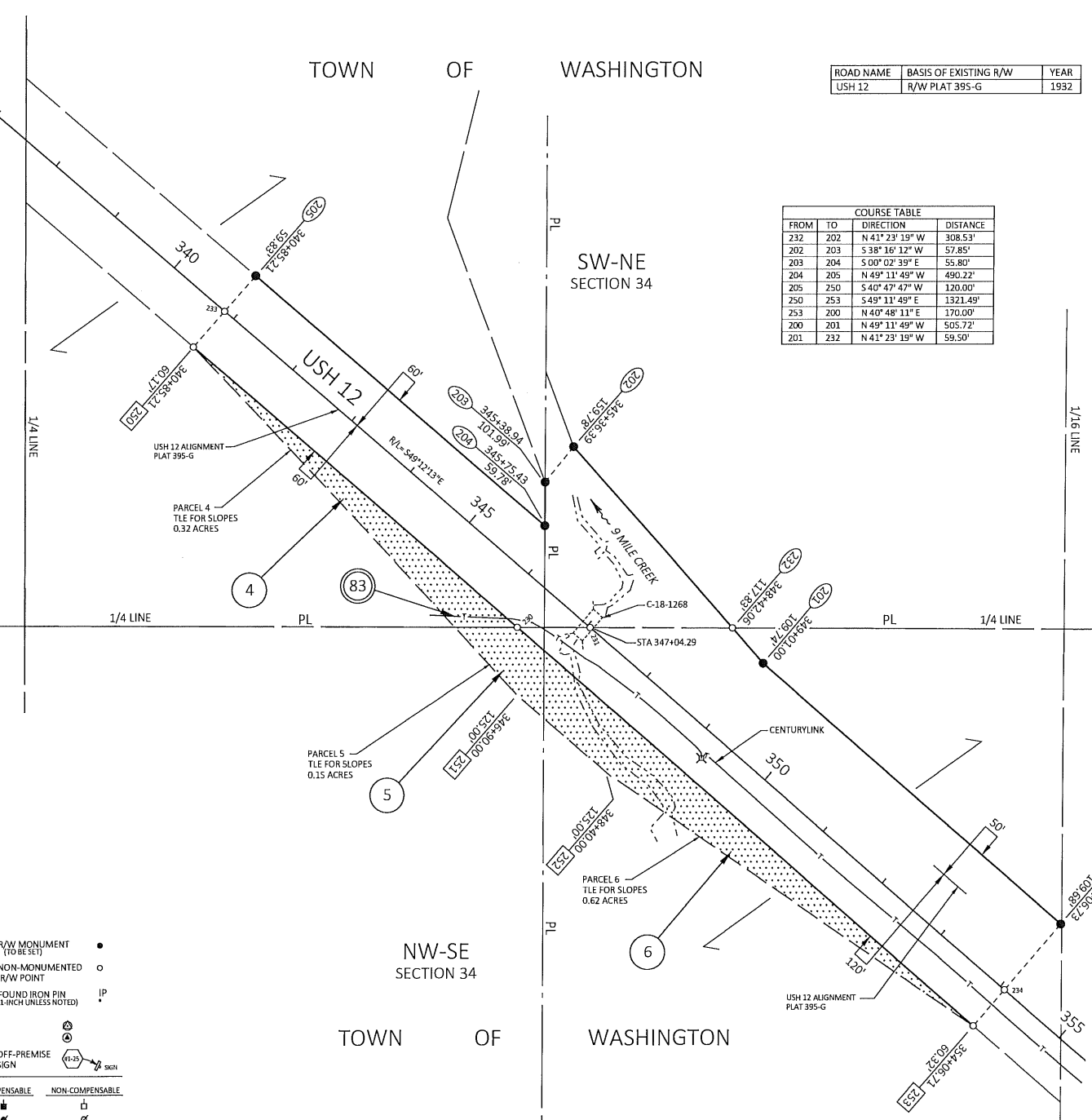
COURSE TABLE (SEGMENTS)				
FROM	TO	DIRECTION	DISTANCE	
205	233	S 40° 47' 47" W	59.83'	
233	250	S 40° 47' 47" W	60.17'	
253	234	N 40° 48' 11" E	60.32'	
234	200	N 40° 48' 11" E	109.68'	
250	230	N 49° 11' 49" E	548.65'	
230	253	N 49° 11' 49" E	772.84'	

COURSE TABLE (QUARTER LINE)				
FROM	TO	DIRECTION	DISTANCE	
53	232	N 89° 44' 34" W	1749.91'	
232	231	N 89° 44' 34" W	181.29'	
231	230	N 89° 44' 34" W	92.66'	
230	52	N 89° 44' 34" W	3306.12'	
53	52	N 89° 44' 34" W	5329.98'	

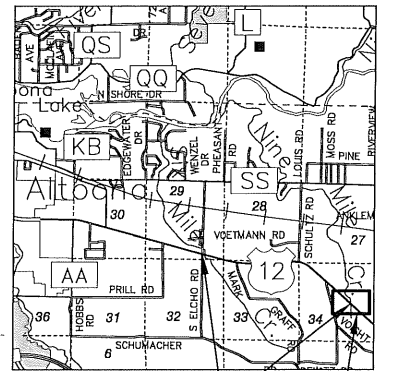
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KATHRYN A. CHRISTENSON
EAU CLAIRE COUNTY WI
REGISTER OF DEEDS
RECORDED ON
04/25/2019 09:02 AM
REC FEE:
TRANS FEE:
EXEMPT #:
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RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 7080-01-22-4.02



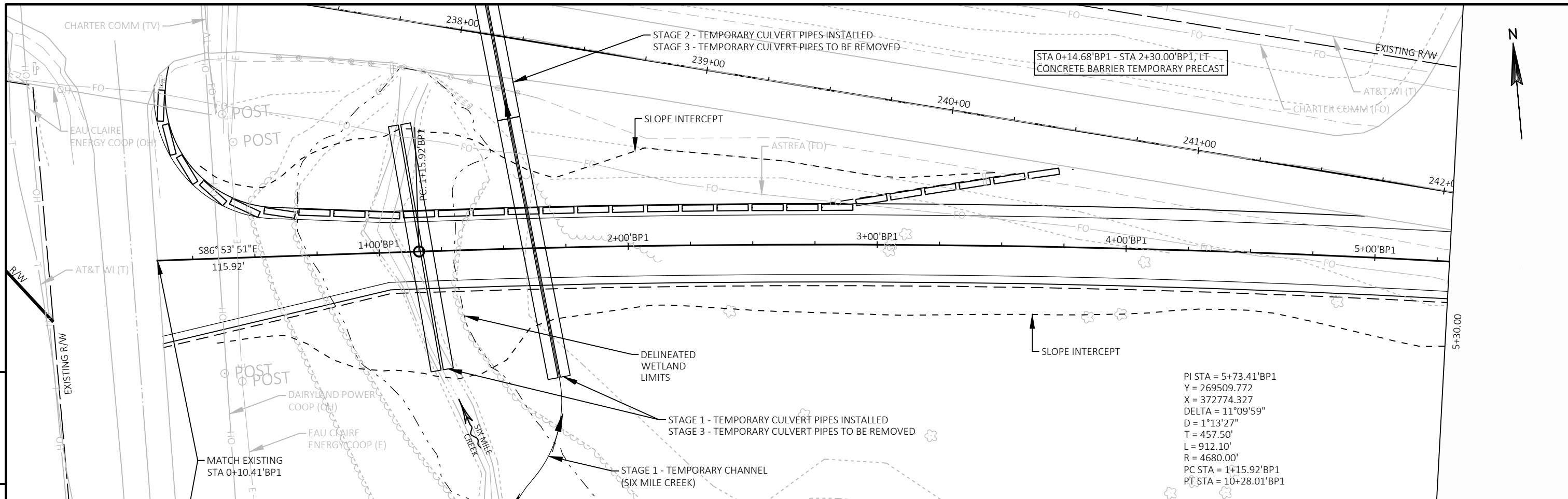
LOCATION SKETCH
NOT TO SCALE



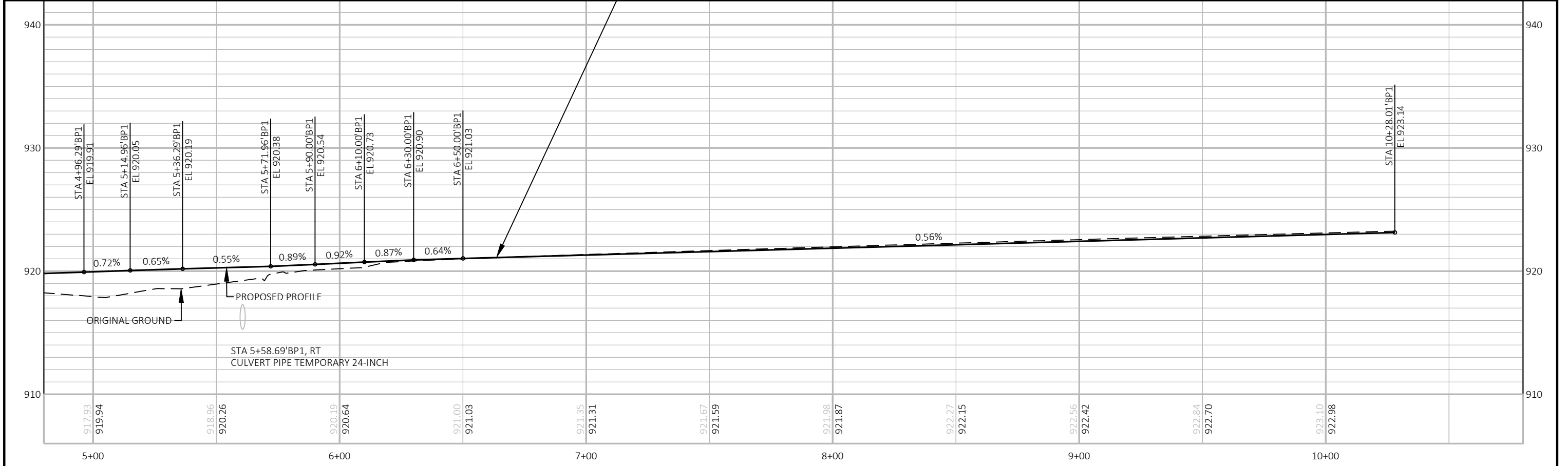
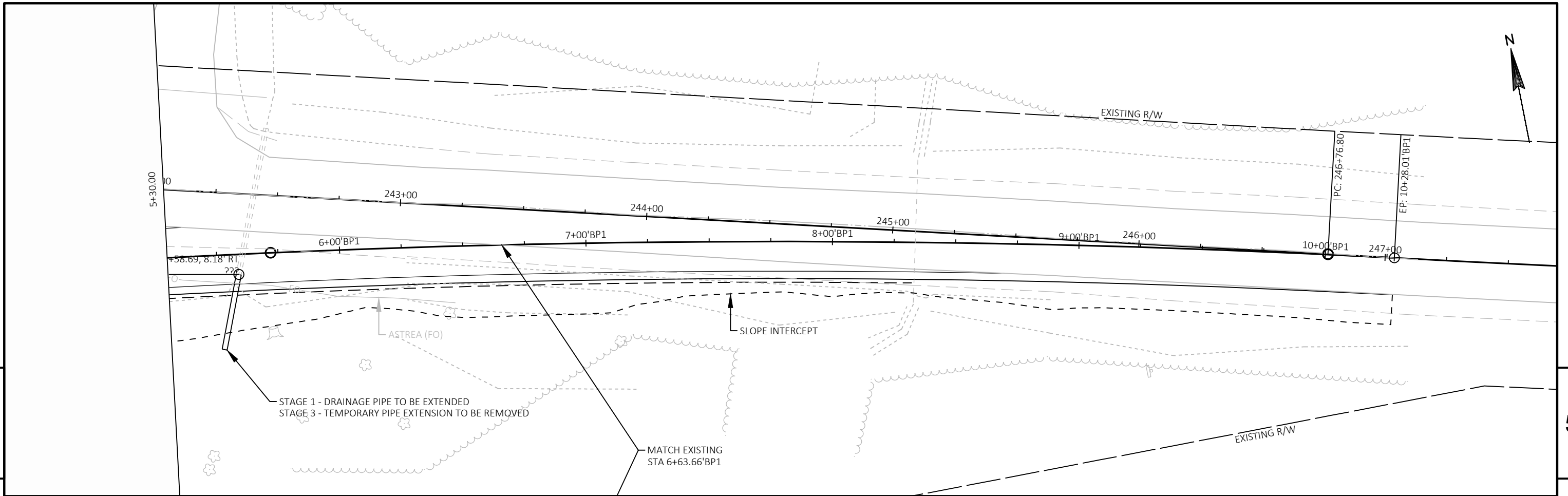
I, NEIL C. BOWE, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



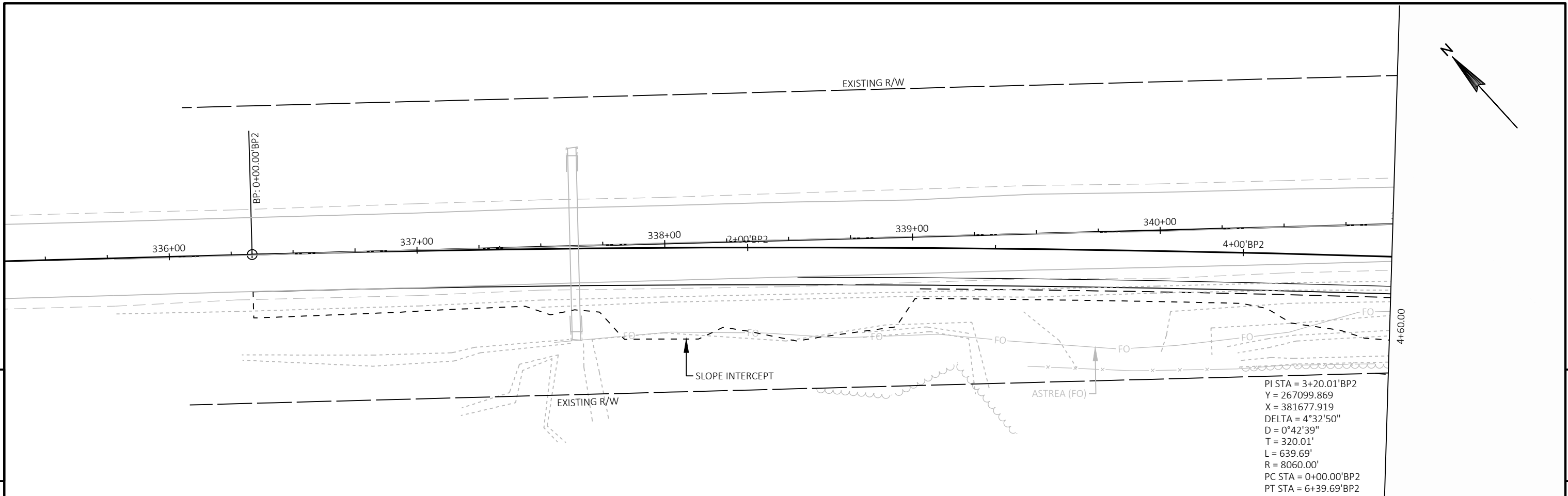
SIGNATURE: *Neil C. Bowe* DATE: 4/22/19
PRINT NAME: NEIL C. BOWE
REGISTRATION NUMBER: S-2827
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION.
SIGNATURE: *Debra B. Stensland* DATE: 4/22/19
PRINT NAME: DEBRA B. STENSLAND



PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	PLAN AND PROFILE: TEMPORARY BYPASSES	SHEET 5
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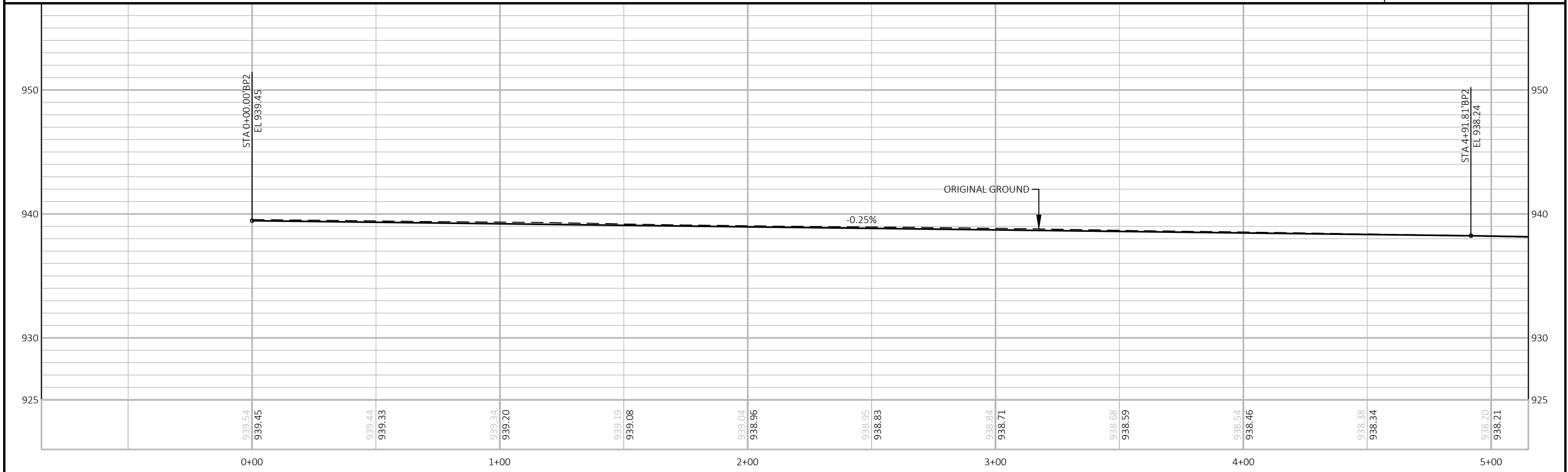


PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE PLAN AND PROFILE: TEMPORARY BYPASSES SHEET 5

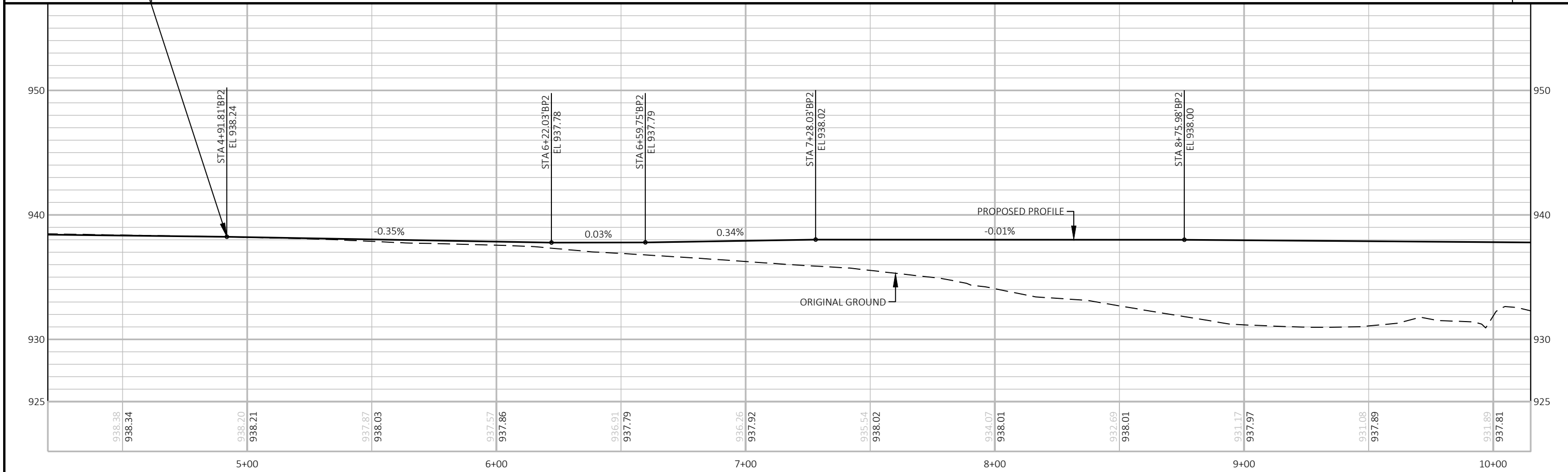
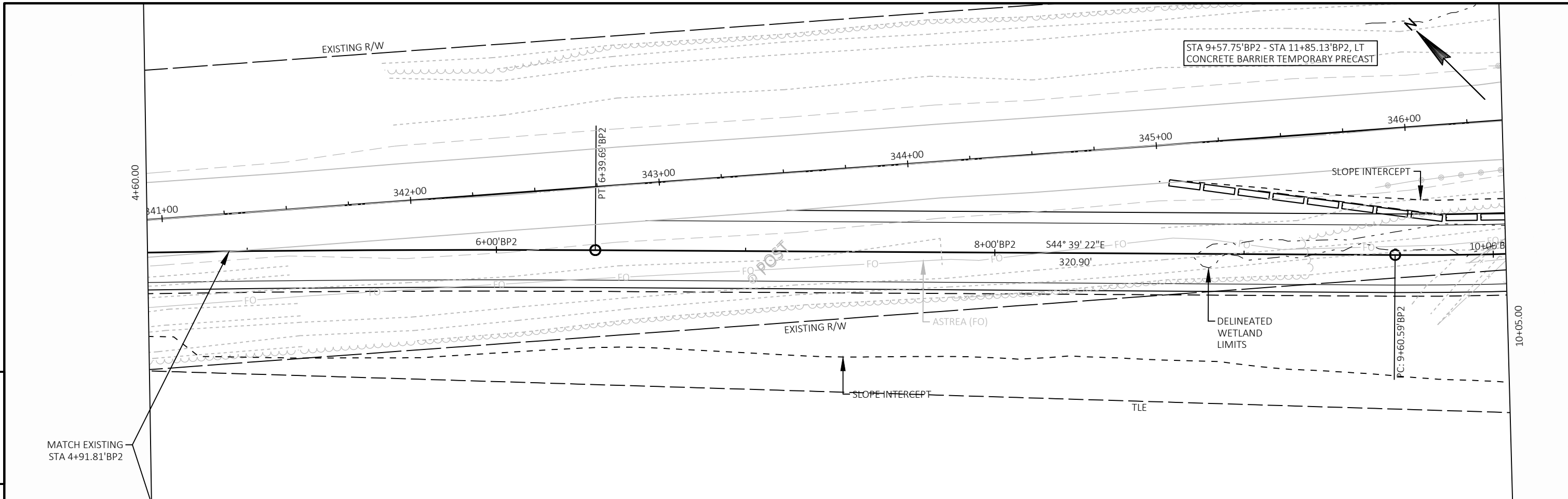


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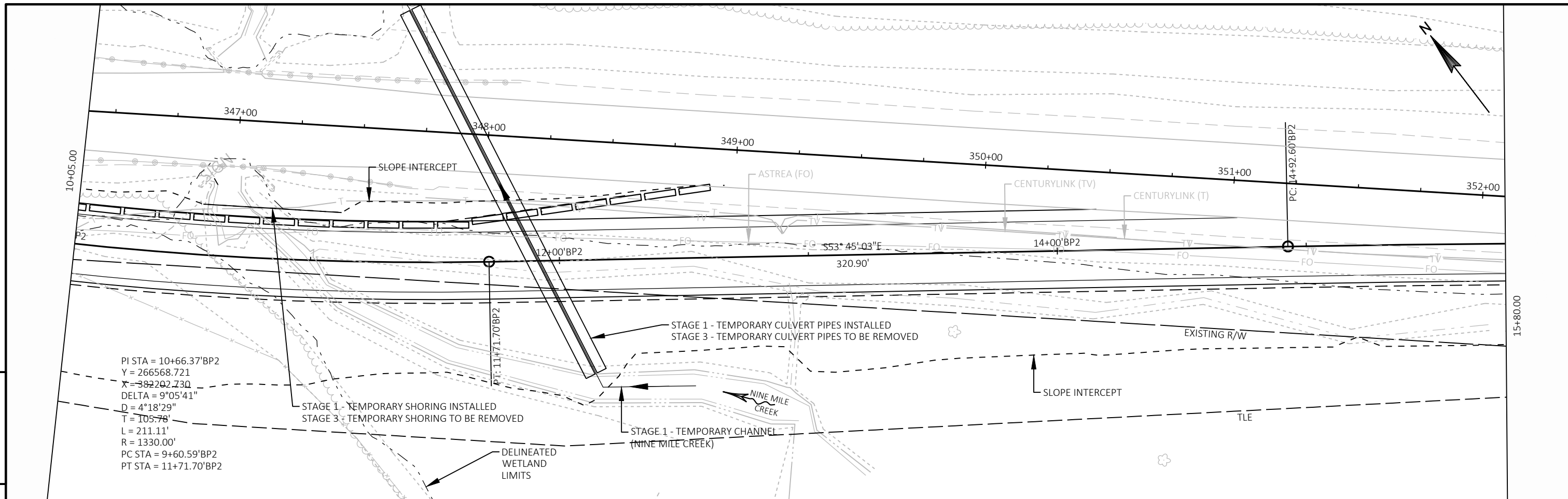
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE PLAN AND PROFILE: TEMPORARY BYPASSES SHEET **E**

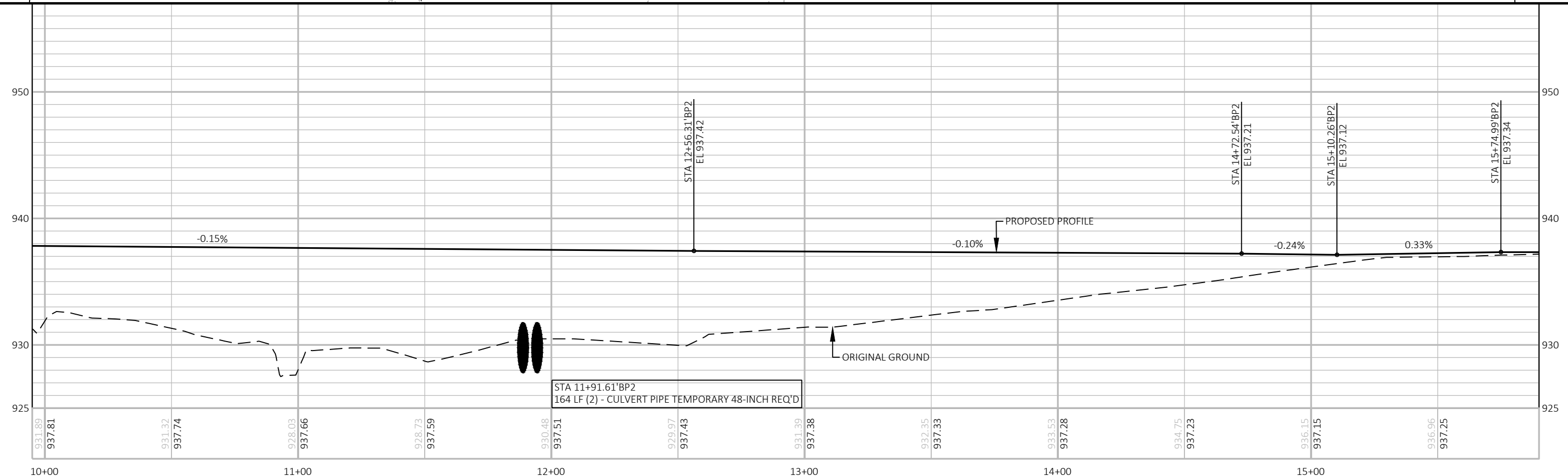


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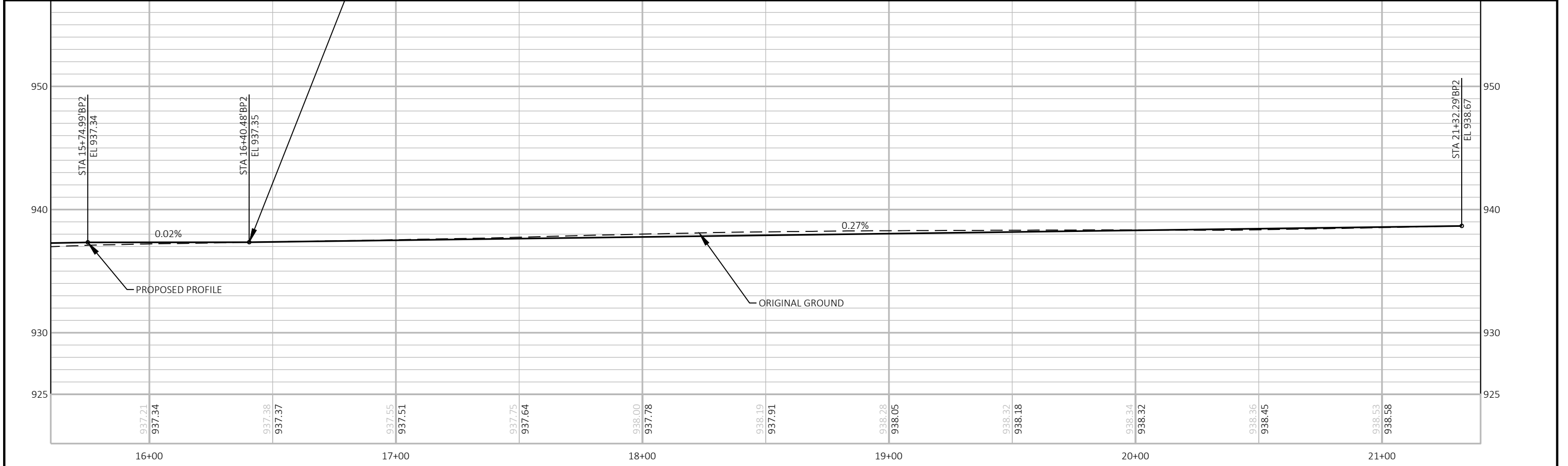
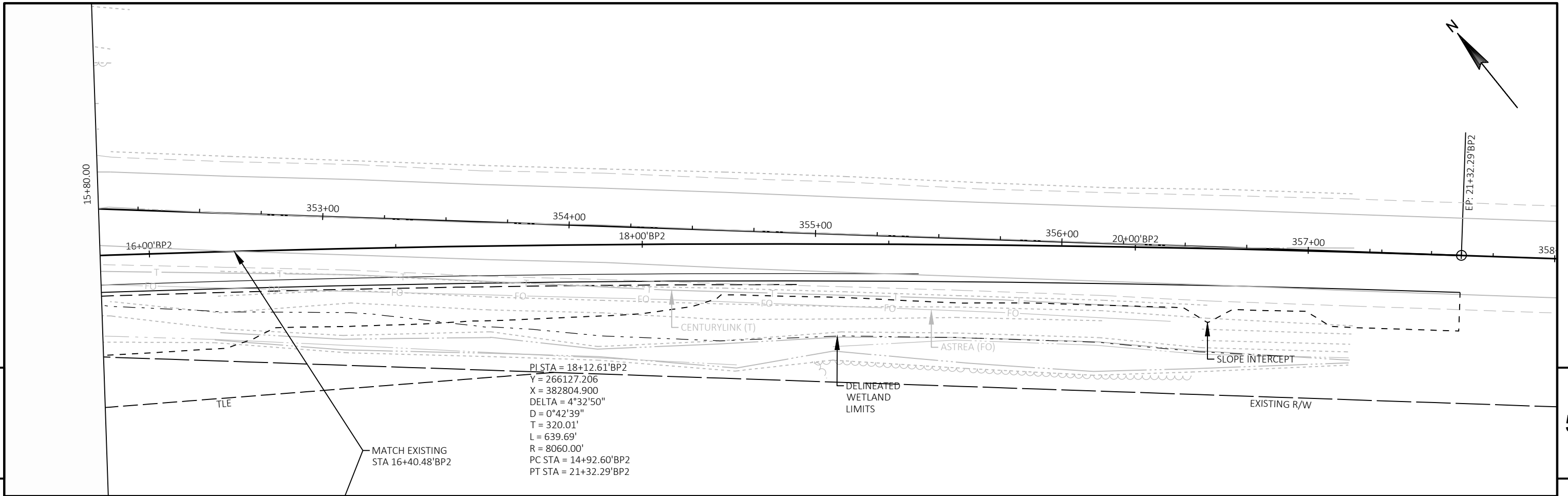


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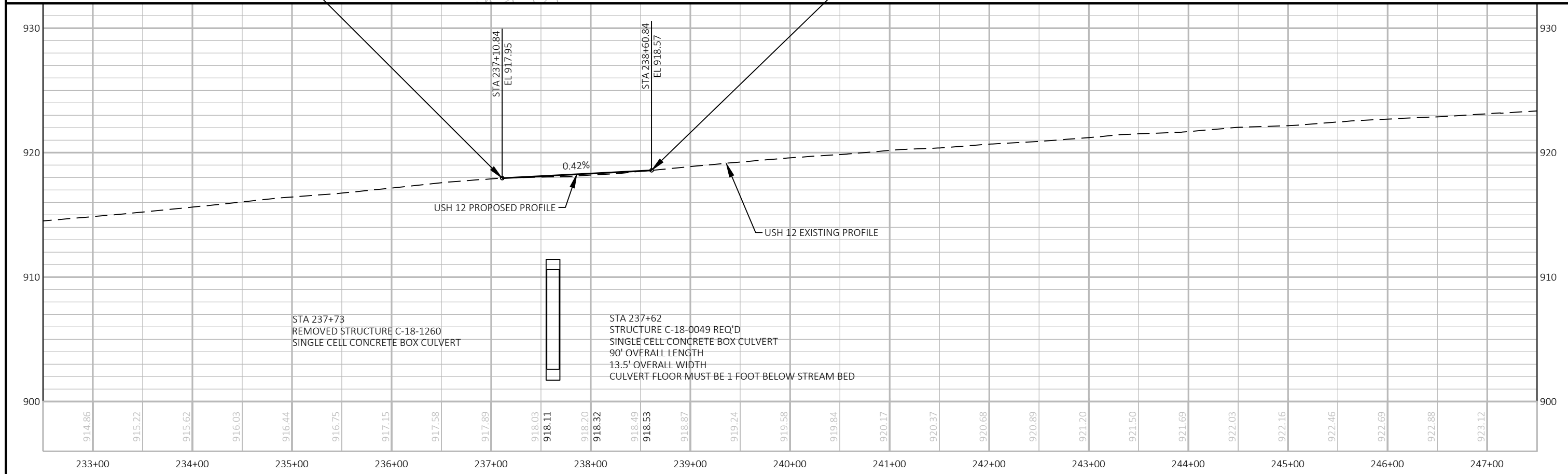
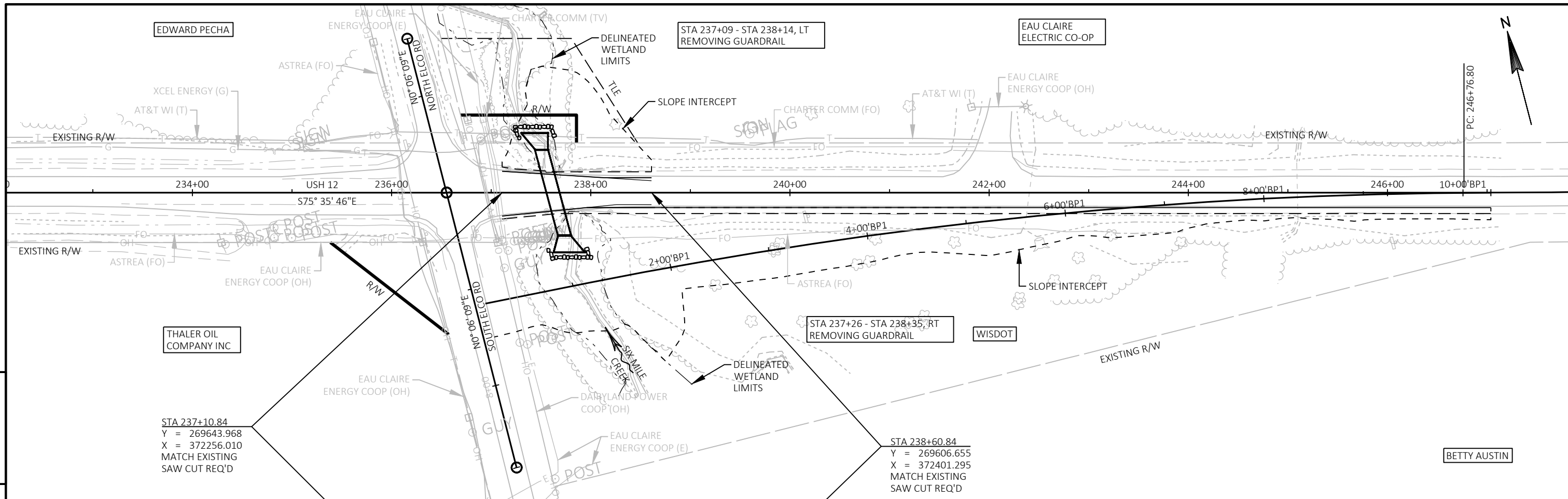
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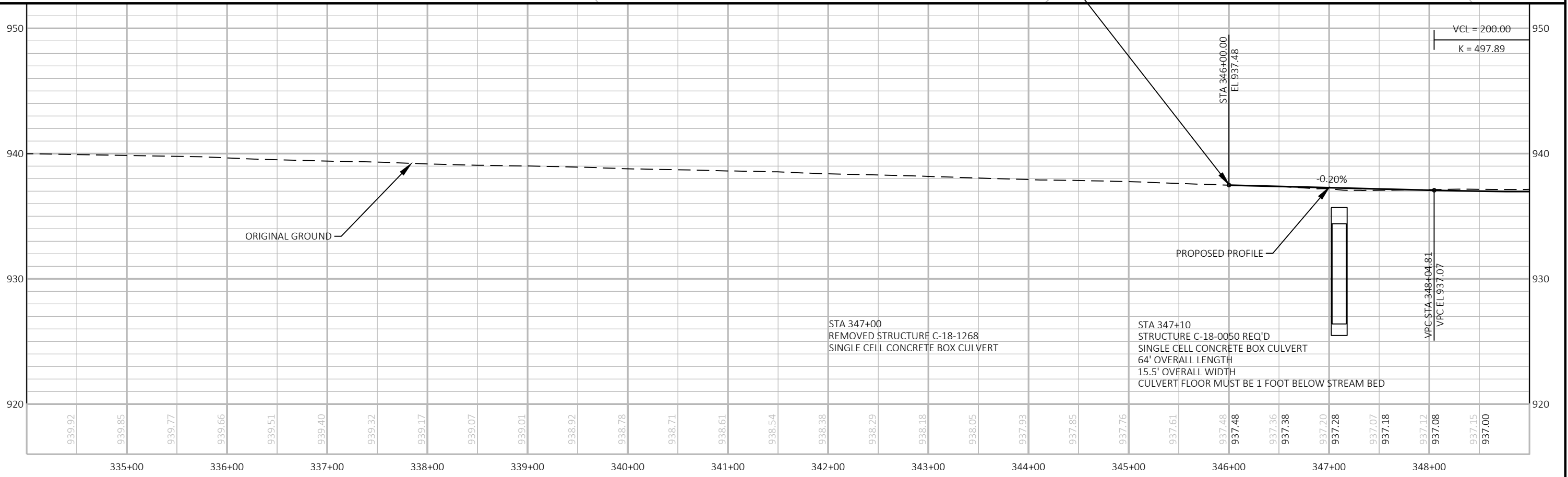
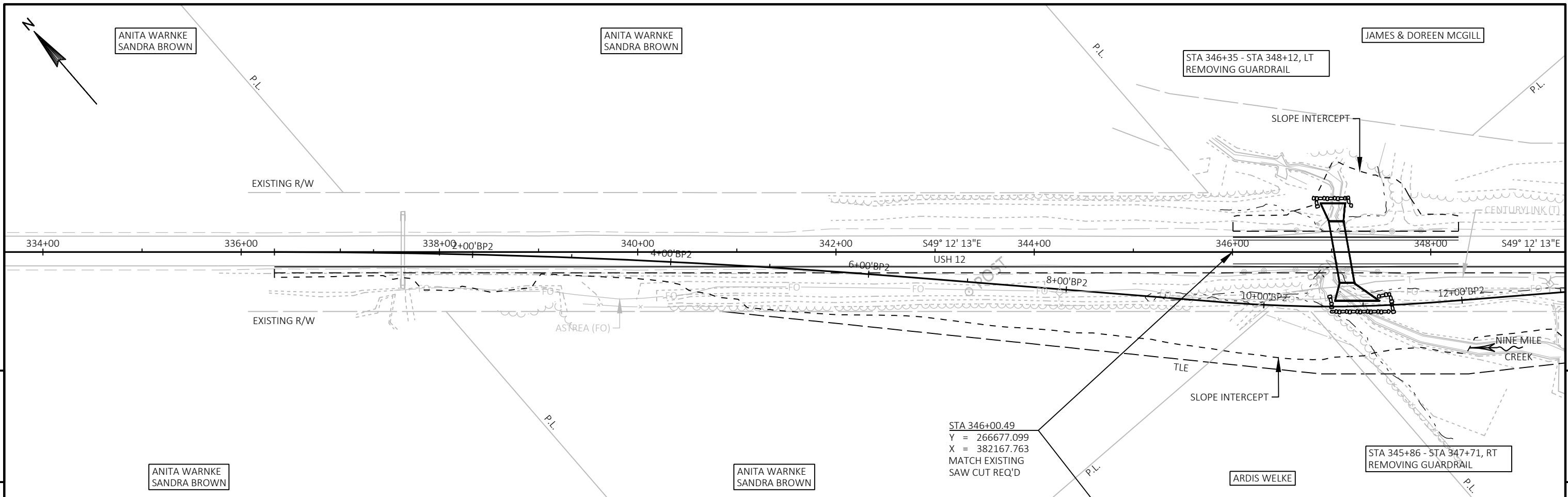
PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE PLAN AND PROFILE: TEMPORARY BYPASSES SHEET: E



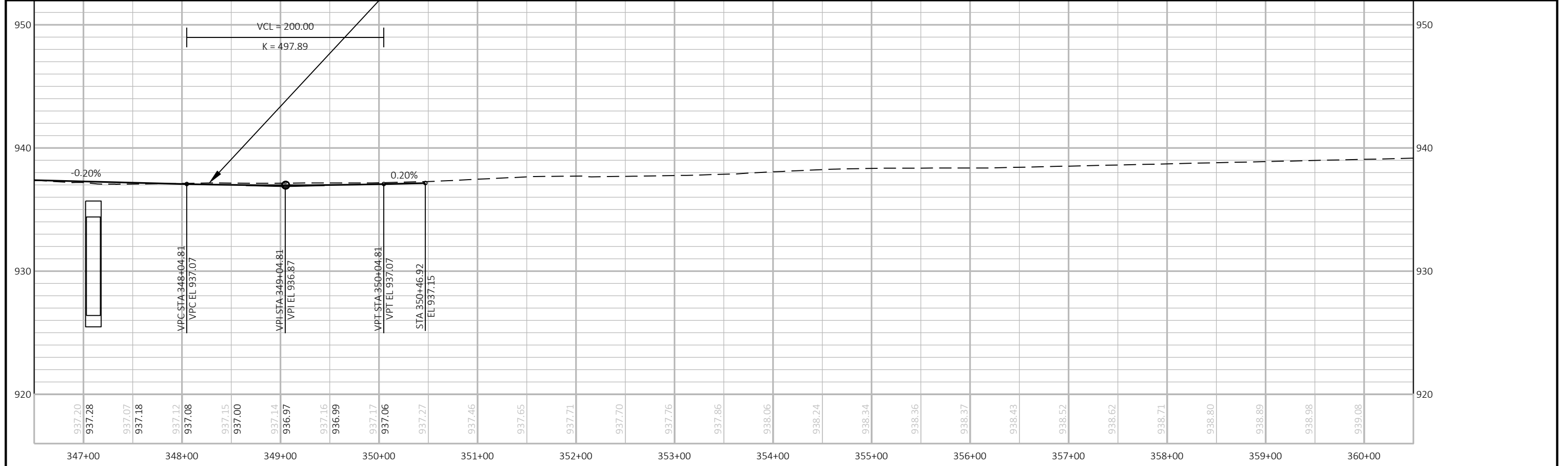
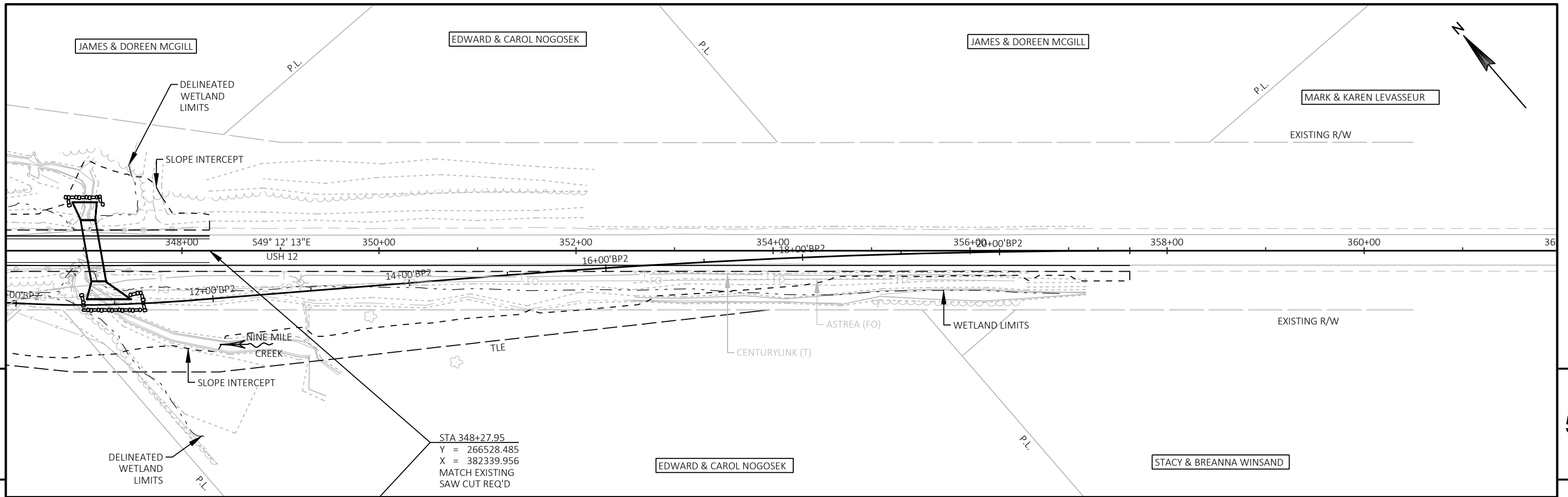
PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE PLAN AND PROFILE: TEMPORARY BYPASSES SHEET: **E**



PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	PLAN AND PROFILE: USH 12	SHEET	E
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE PLAN AND PROFILE: USH 12 SHEET: 5

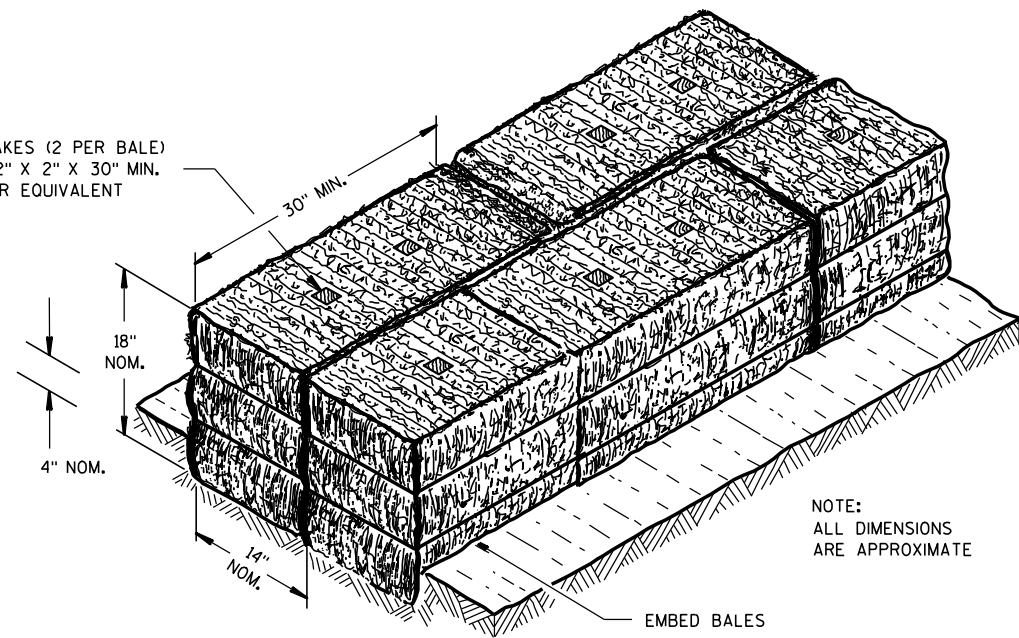


PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE PLAN AND PROFILE: USH 12 SHEET: 5

Standard Detail Drawing List

08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
13A08-01	ASPHALTIC RUMBLE STRIPS AT INTERSECTION
14B07-15A	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15B	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15C	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15D	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15E	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15F	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15G	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15H	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15I	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
15A04-05A	FLEXIBLE DELINEATOR POST
15A04-05B	DELINEATOR BRACKET WITH REFLECTIVE SHEETING
15A04-05C	DELINEATOR POST WITH REFLECTIVE SHEETING
15B01-08A	FENCE WOVEN WIRE
15B01-08B	FENCE WOVEN WIRE
15C02-07A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-07B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C35-03A	PAVEMENT MARKING (INTERSECTIONS)
15D28-03	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D31-03	TRAFFIC CONTROL, TEMPORARY BYPASS ROADWAY
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS

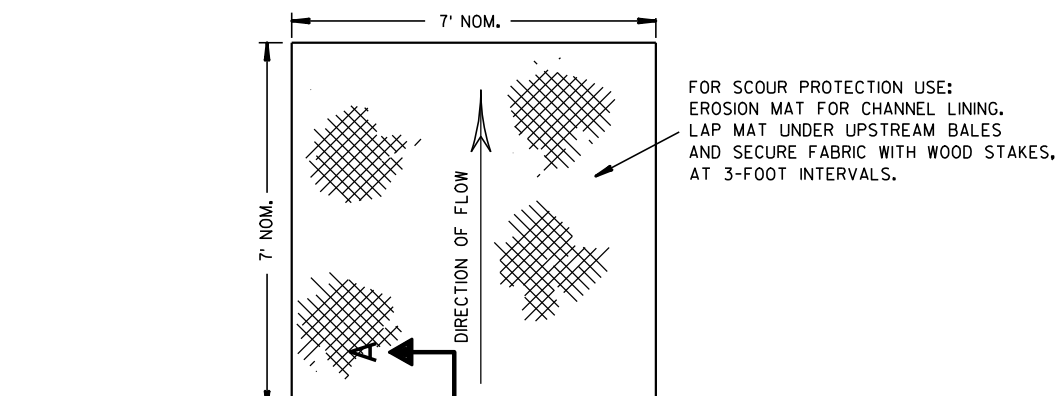
WOOD STAKES (2 PER BALE)
NOMINAL 2" X 2" X 30" MIN.
LENGTH OR EQUIVALENT



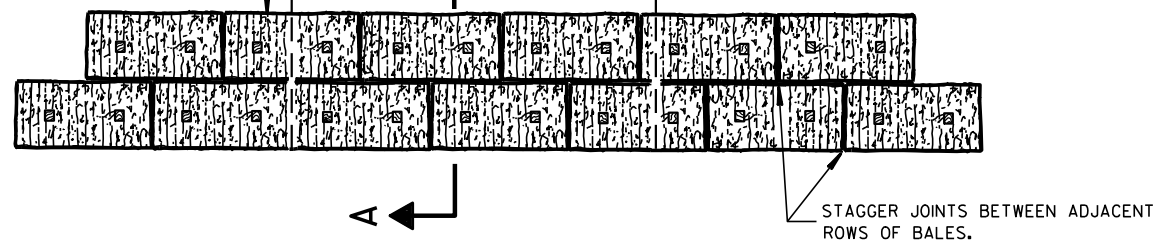
NOTE:
ALL DIMENSIONS
ARE APPROXIMATE

EMBED BALES

SECTION A-A

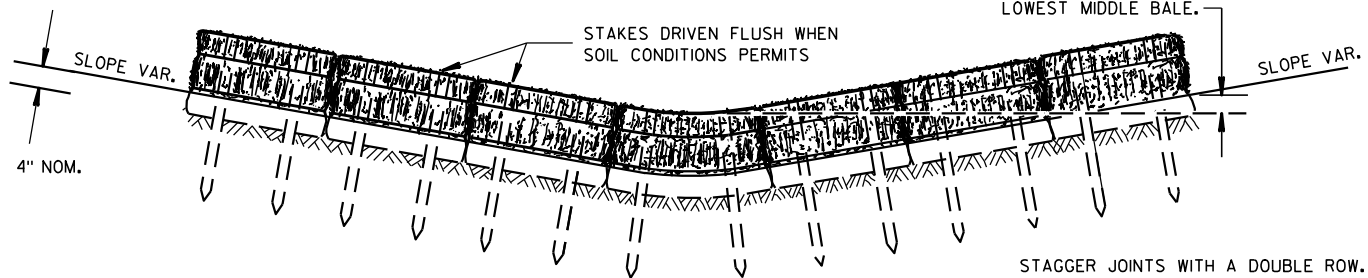


FOR SCOUR PROTECTION USE:
EROSION MAT FOR CHANNEL LINING.
LAP MAT UNDER UPSTREAM BALES
AND SECURE FABRIC WITH WOOD STAKES,
AT 3-FOOT INTERVALS.



PLAN VIEW

BOTTOM ELEVATION OF END BALE SHALL
BE EQUAL TO OR GREATER THAN TOP OF
LOWEST MIDDLE BALE.



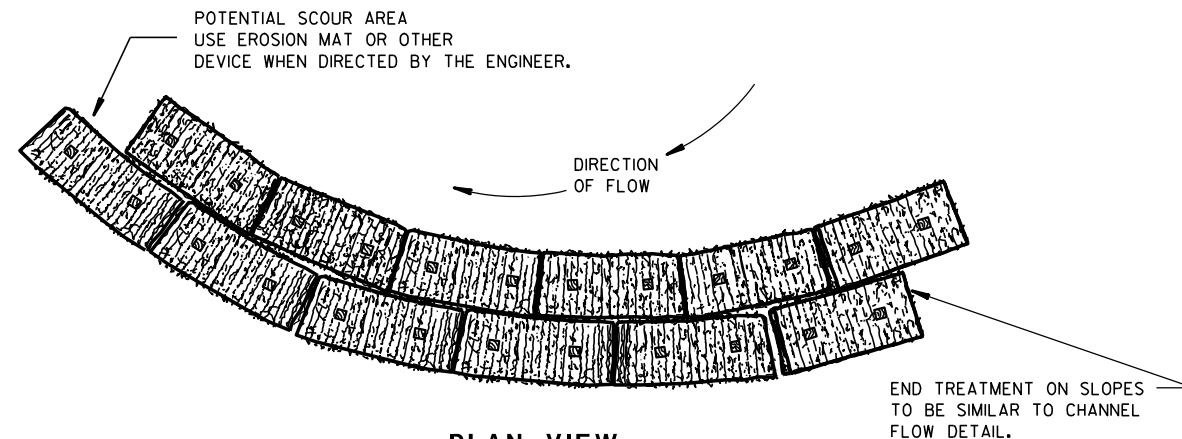
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

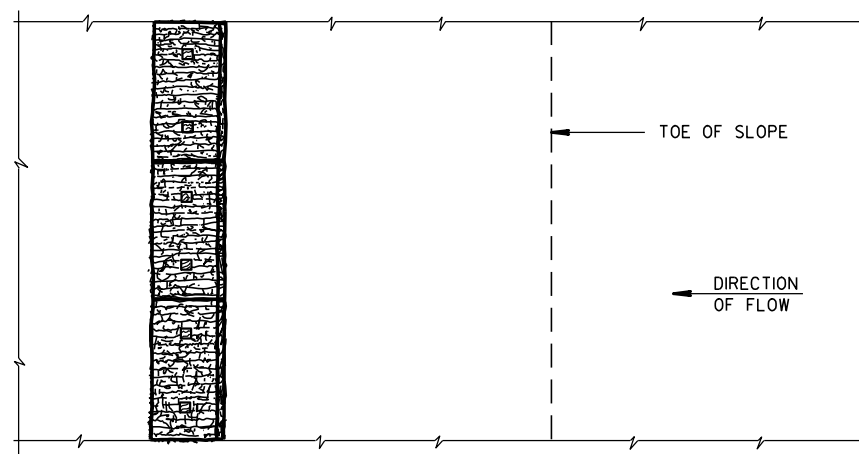
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

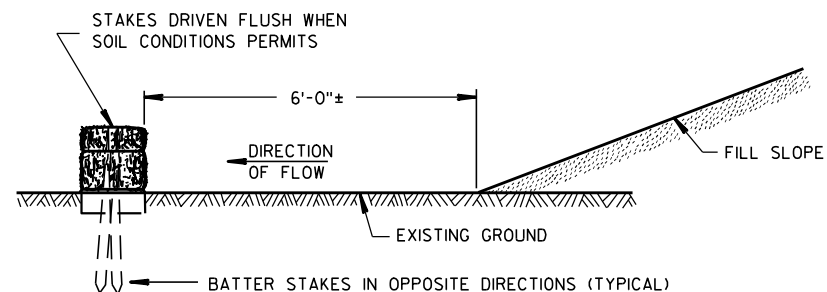


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

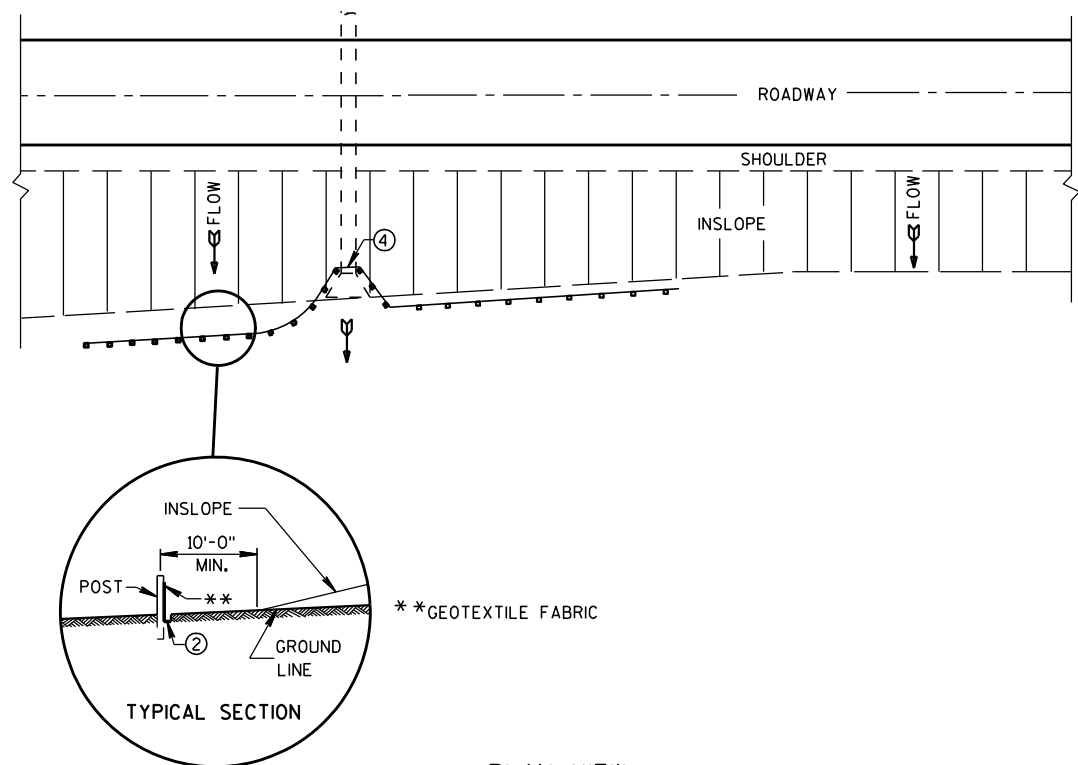
WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

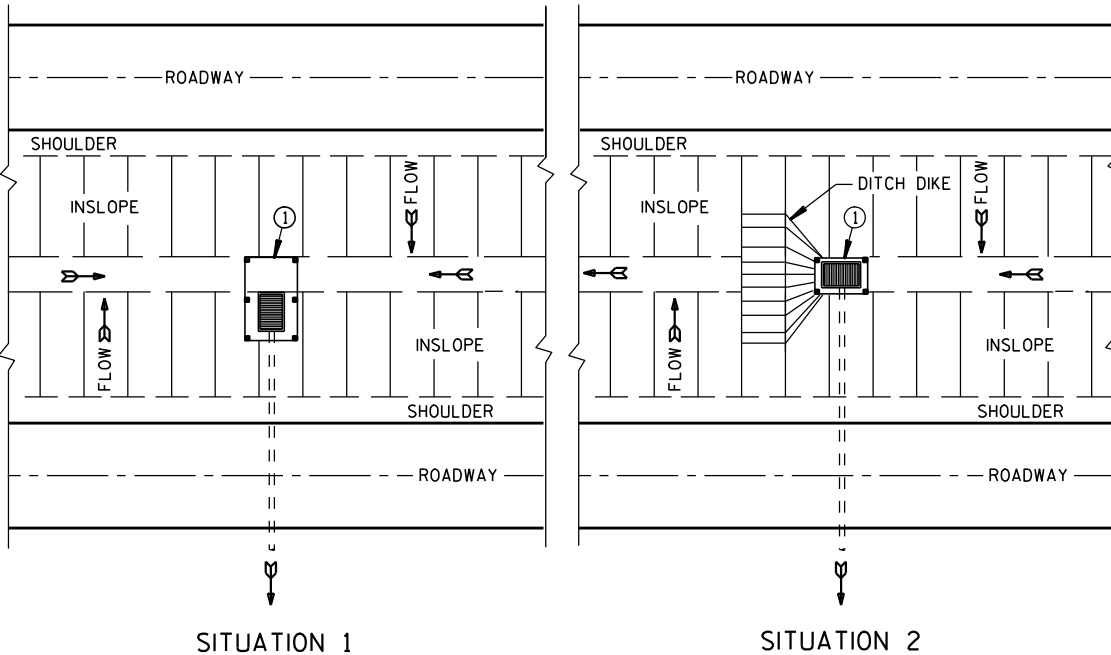
TYPICAL INSTALLATIONS OF
EROSION BALES / TEMPORARY
DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

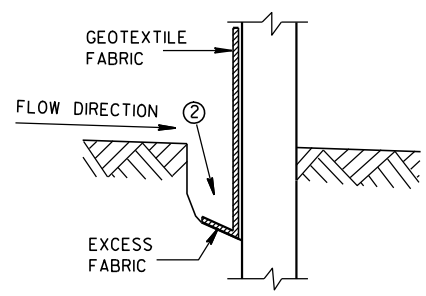


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

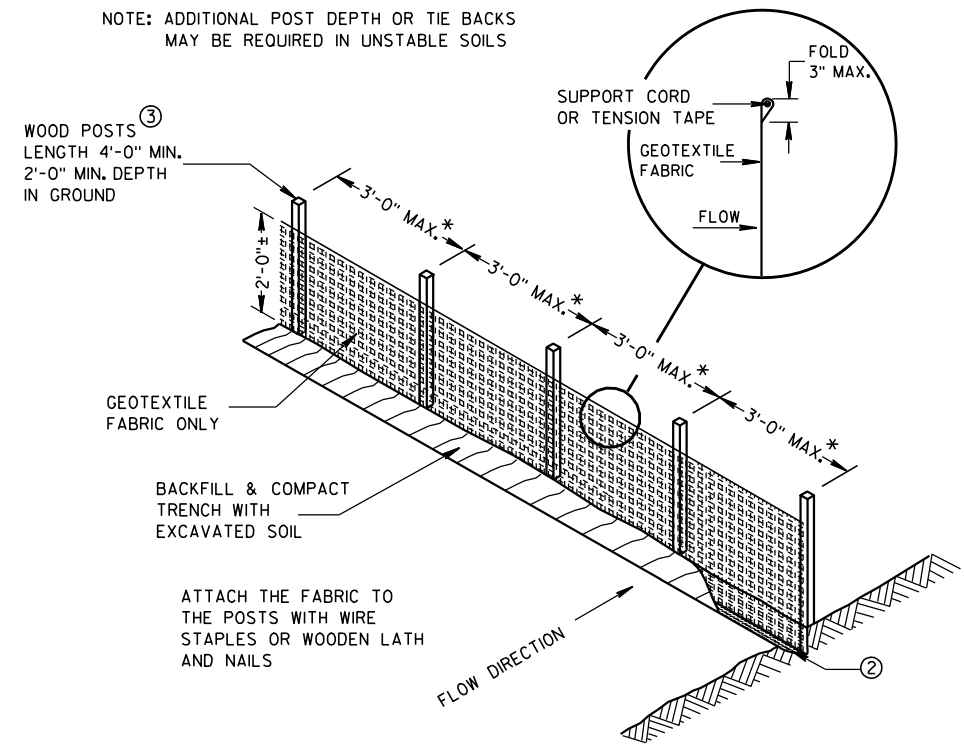
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



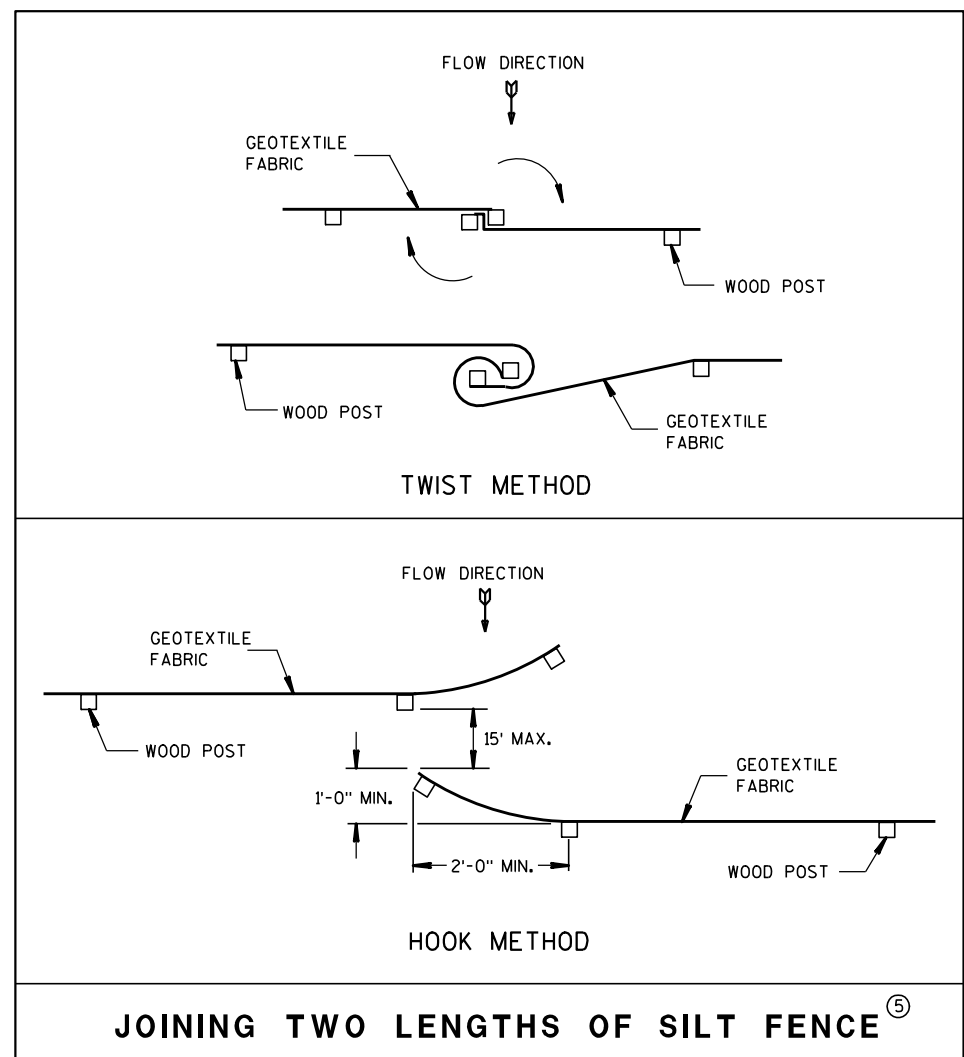
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

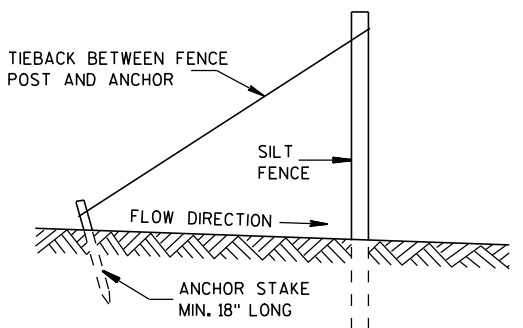


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

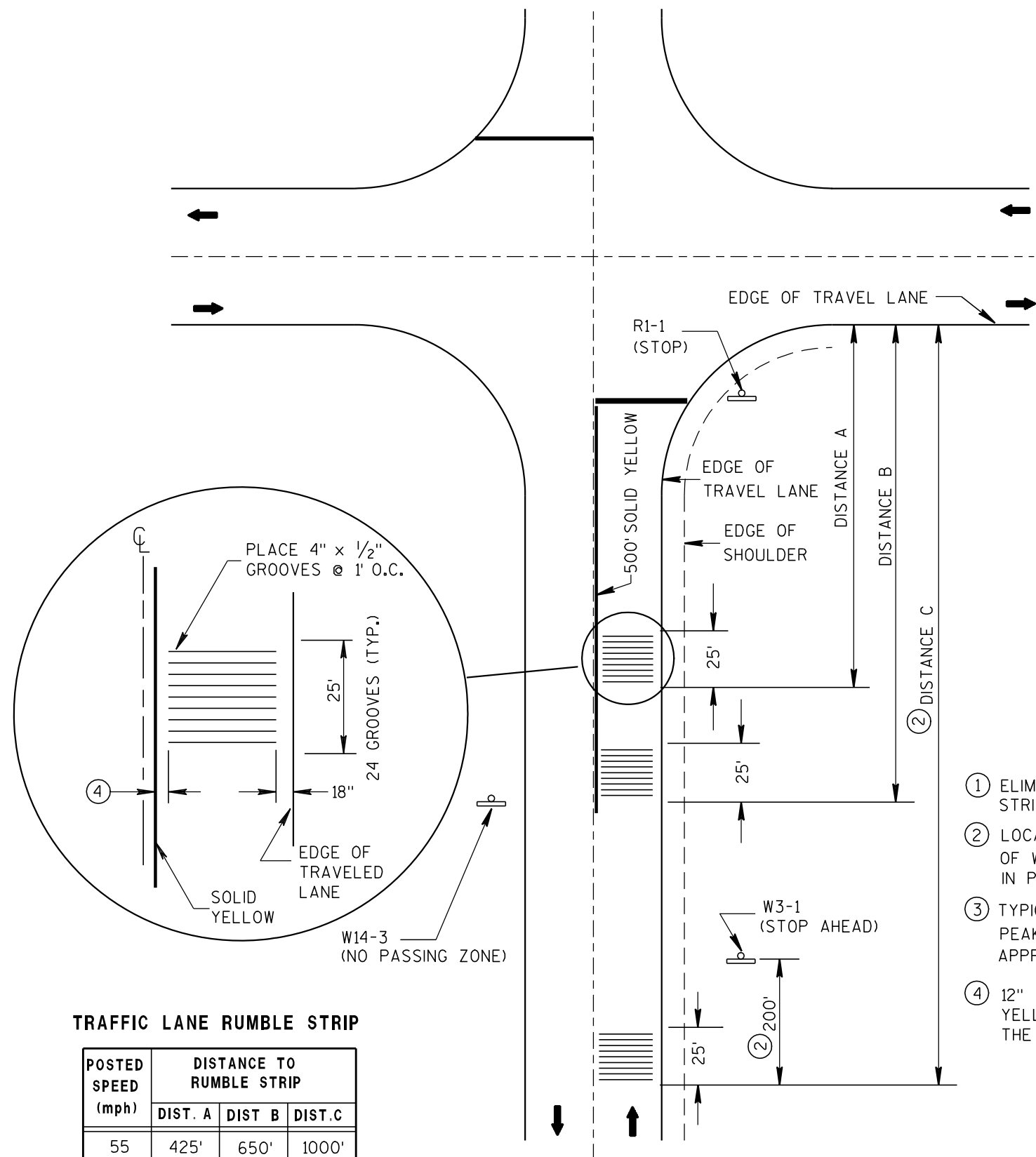


JOINING TWO LENGTHS OF SILT FENCE



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



TRAFFIC LANE RUMBLE STRIP

POSTED SPEED (mph)	DISTANCE TO RUMBLE STRIP		
	DIST. A	DIST. B	DIST. C
55	425'	650'	1000'
50	325'	450'	800'
45	275'	400'	650'
40	225'	①	550'
35	175'	①	475'
≤ 30	125'	①	425'

ARROW SYMBOL (➔) SHOWS DIRECTION OF TRAVEL

**PLAN VIEW
RUMBLE STRIP LOCATION**

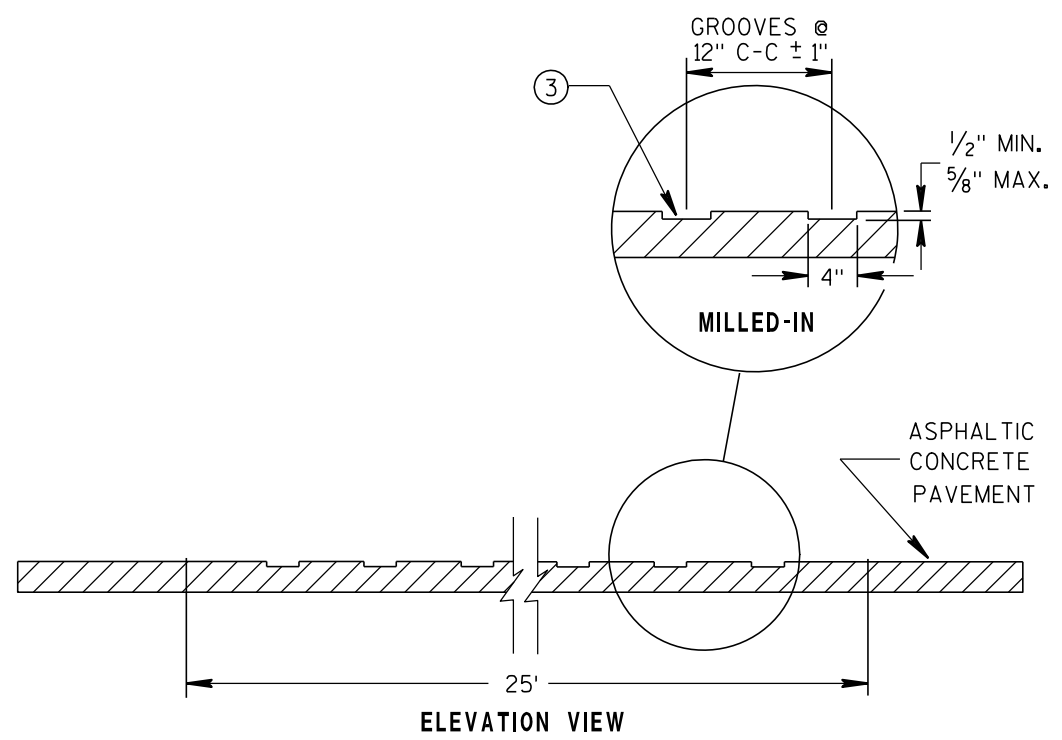
- ① ELIMINATE THE MIDDLE SET OF RUMBLE STRIPS.
- ② LOCATE RUMBLE STRIP 200' IN ADVANCE OF W3-1 SIGN AS SHOWN. IF W3-1 IS NOT IN PLACE, USE DISTANCE C.
- ③ TYPICAL VERTICAL VARIATION BETWEEN PEAKS AND VALLEYS WITHIN THE CUT APPROXIMATELY 1/16"
- ④ 12" CLEAR BETWEEN THE SOLID YELLOW LINE AND THE EDGE OF THE RUMBLE.

GENERAL NOTES

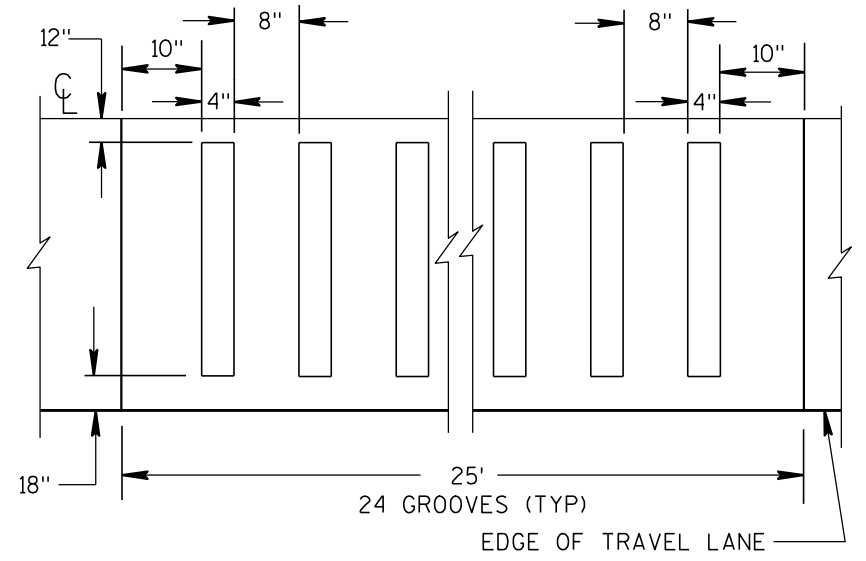
CONTRACTOR SHALL CONFIRM RUMBLE STRIP LOCATION WITH THE ENGINEER PRIOR TO INSTALLATION. THE ENGINEER MAY MODIFY THE RUMBLE STRIP LOCATION AS FIELD CONDITIONS DICTATE.

WHEN ASPHALTIC PAVEMENT IS NEW IN THE RUMBLE AREA THE CONTRACTOR SHALL ALLOW THE PAVEMENT TO CURE A MINIMUM OF 7 DAYS PRIOR TO RUMBLE INSTALLATION.

PAVEMENT MARKING AND SIGNING DETAILS AND SPECIFICATIONS ARE PROVIDED ELSEWHERE IN THE CONTRACT.

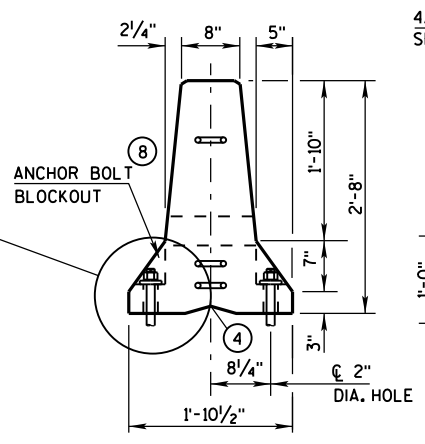
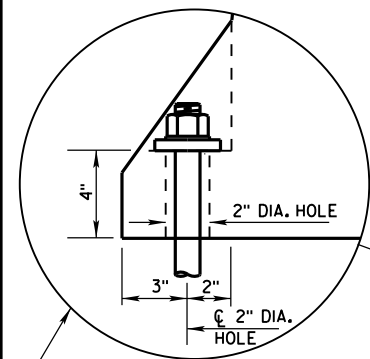


ELEVATION VIEW

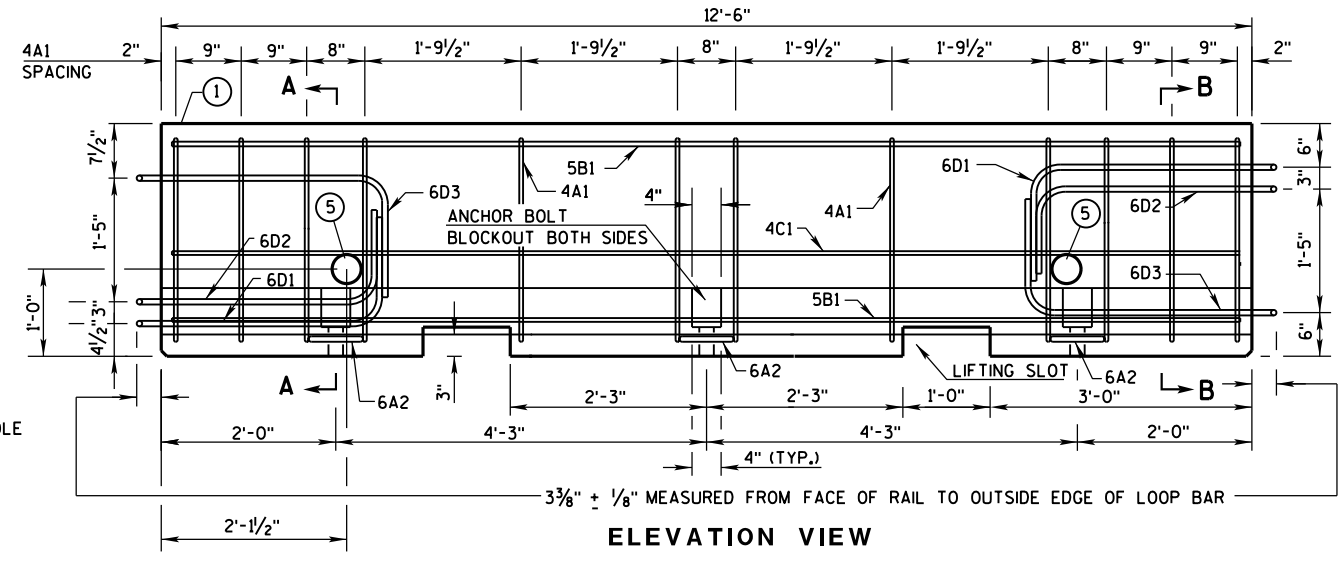


**PLAN VIEW
ASPHALTIC PAVEMENT
MILLED-IN**

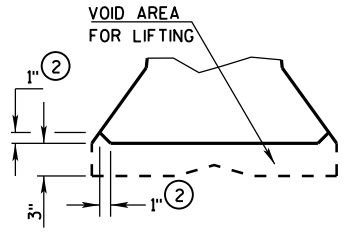
ASPHALTIC RUMBLE STRIPS AT INTERSECTION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/17/2011 DATE	/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



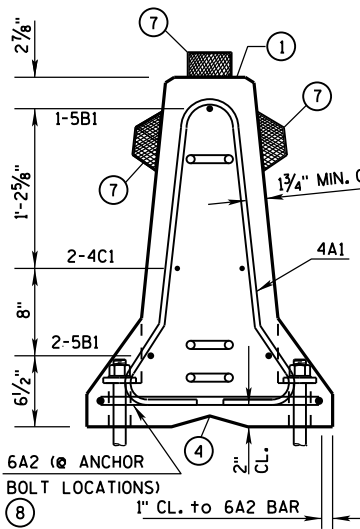
END VIEW



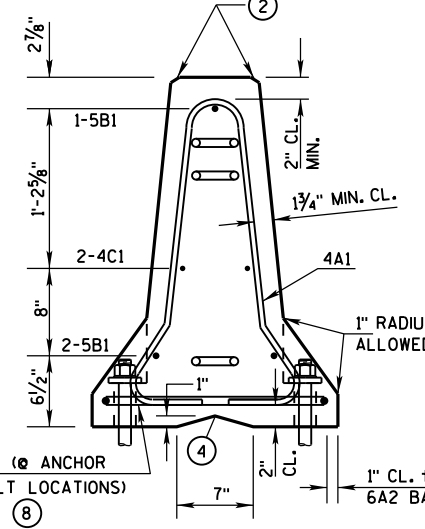
ELEVATION VIEW



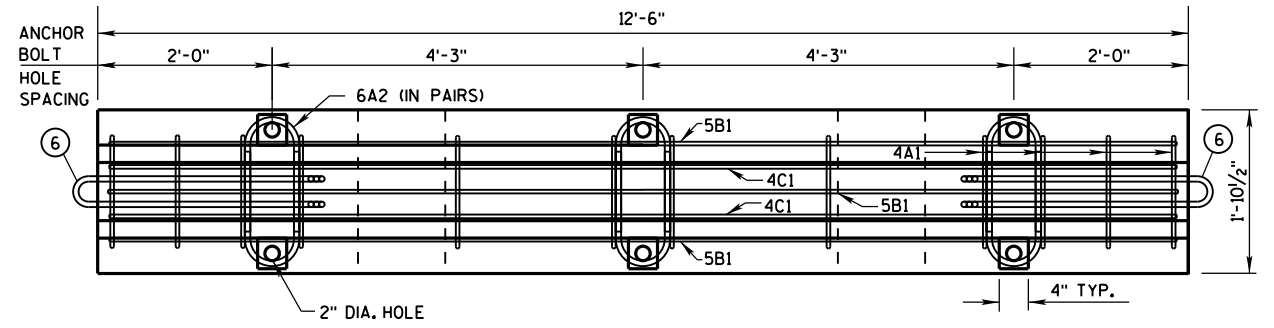
DETAIL "B"
LIFTING SLOT DETAIL



SECTION A-A
(STIRRUP PLACEMENT)

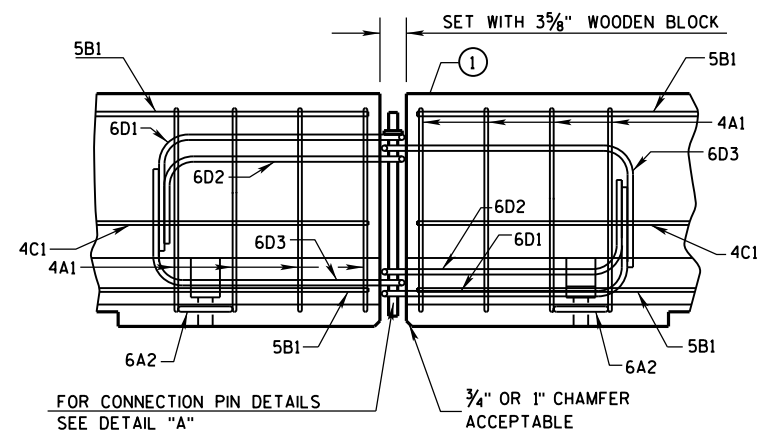


SECTION B-B
(STIRRUP PLACEMENT)

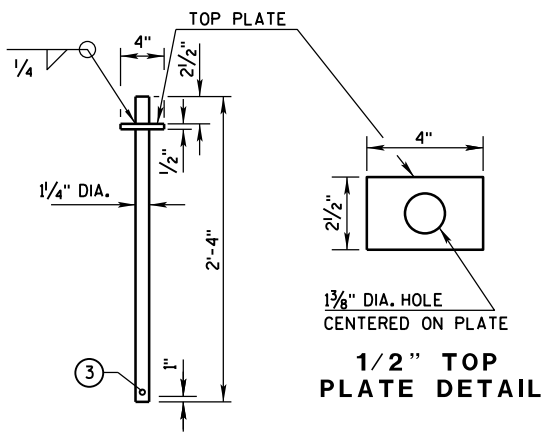


PLAN VIEW

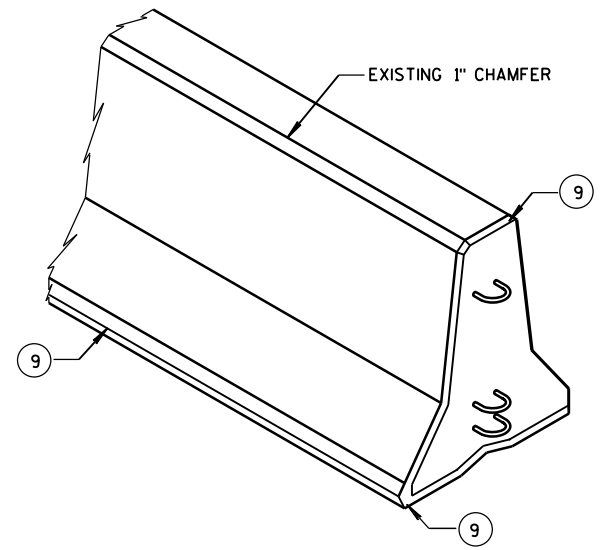
DETAILS OF BARRIER SECTION



DETAILS OF BARRIER CONNECTION



DETAIL "A"
CONNECTION PIN
(A36 STEEL (10.9 LB EACH))



GENERAL NOTES

THESE GENERAL NOTES APPLY TO SHEETS 14B7-15(d) THRU 14B7-15(i).

DO NOT INTERMIX CONCRETE BARRIER TEMPORARY PRECAST, 12'-6" (CBTP12.5) WITH OTHER TEMPORARY CONCRETE BARRIERS.

USE ASTM A-615, GRADE 60, DEFORMED STEEL BARS FOR BARS 4A1, 6A2, 5B1 AND 4C1 IN THE BARRIER SECTION AND FOR 4V1, 4V2, 4V3, 4V4, 4V5, 4V6, 4F1, 4F2 AND 5F3 IN THE BARRIER TAPER SECTION.

LOOP BARS 6D1, 6D2 AND 6D3 SHALL BE 3/4" SMOOTH STEEL BARS WITH A MINIMUM YIELD STRENGTH OF 60 KSI, A TENSILE STRENGTH OF NOT LESS THAN 1.25 TIMES THE YIELD STRENGTH BUT A MINIMUM OF 80 KSI, A MINIMUM 14% ELONGATION IN 8 INCHES AND PASSING A 180 DEGREE BEND TEST USING A 3-1/2" PIN BEND DIAMETER FOR BEND TESTS. THE LOOPS SHALL BE INSTALLED WITHIN 1/8" OF THE PLAN DIMENSION.

CONSTRUCT LIFTING SLOTS AS SPECIFIED ON THE PLANS TO FACILITATE THE DRAINAGE OF WATER AFTER INSTALLATION.

PLACE BARRIER ON A PAVED SURFACE. REMOVE ALL LOOSE DIRT AND SAND FROM THE ROADWAY SURFACE PRIOR TO PLACEMENT OF THE BARRIER.

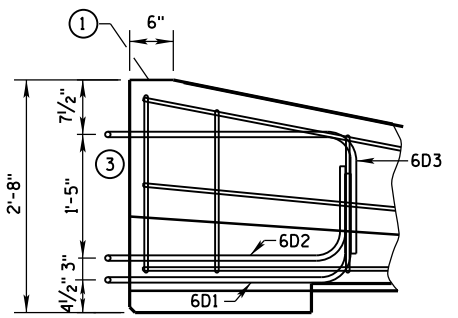
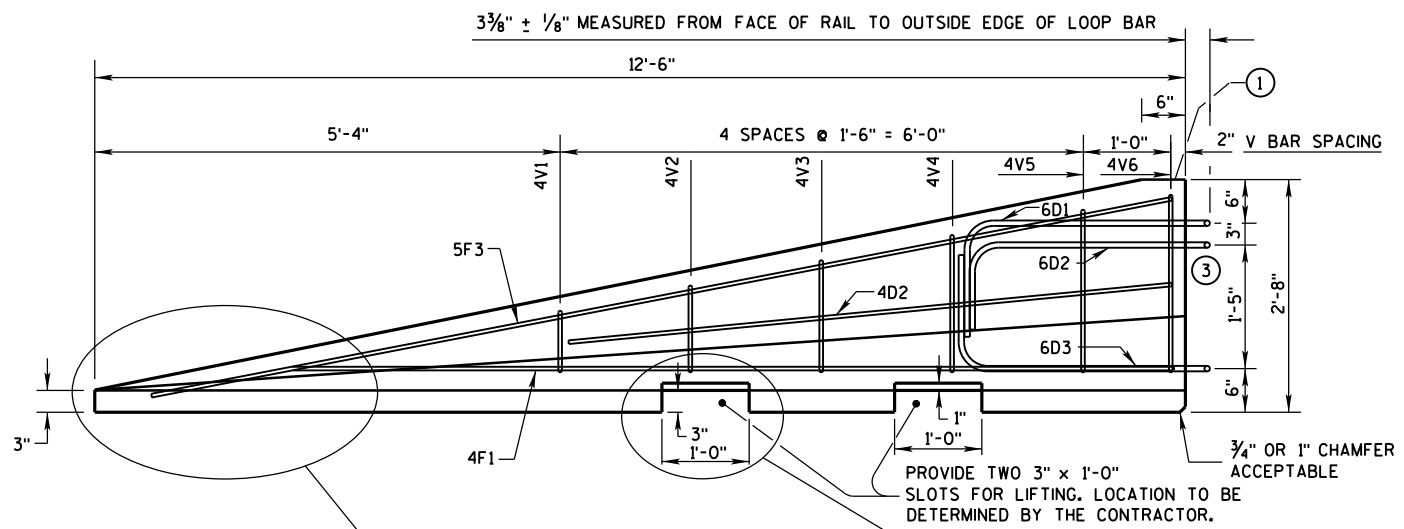
INSTALL MECHANICAL OR ADHESIVE ANCHORS PER MANUFACTURER'S RECOMMENDATIONS. PROVIDE MANUFACTURER'S INFORMATION TO PROJECT ENGINEER.

- ① MARK ONE END OF EACH BARRIER PERMANENTLY BY FORMING INTO THE BARRIER THE FOLLOWING INFORMATION:
 - a. TYPE: WICBTP
 - b. MANUFACTURER
 - c. DATE MANUFACTURED (MONTH AND YEAR)
- ② 1" CHAMFER TO PREVENT SPALLING.
- ③ A 3/8" HOLE IN THE CONNECTION PIN, AT THE LOCATION SHOWN, IS ACCEPTABLE, BUT NOT REQUIRED..
- ④ "V" NOTCH IS OPTIONAL.
- ⑤ THE 4" DIAMETER, 11 GAUGE STEEL, ROUND MECHANICAL TUBING SLEEVE FOR LIFTING (OPTIONAL).
- ⑥ NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.
- ⑦ USE DELINEATORS CONFORMING TO SECTION 633 OF THE STANDARD SPECIFICATIONS. CONTRACTOR MAY USE ALTERNATE SHAPES AND HOUSING. INSTALL DELINEATORS ACCORDING TO MANUFACTURERS INSTRUCTION. INSTALL YELLOW REFLECTORS WHEN BARRIER IS LOCATED TO THE LEFT OF TRAFFIC AND WHITE REFLECTORS WHEN BARRIER IS LOCATED TO THE RIGHT OF TRAFFIC. SPACE DELINEATORS A MAXIMUM OF 25 FEET APART. PROVIDE TOP MOUNTED DELINEATORS IN ADDITION TO THE SIDE MOUNTED DELINEATORS ON ALL BARRIER INSTALLATIONS LOCATED ON A CURVED ALIGNMENT LONGER THAN 200 FEET AND ON BARRIERS USED TO SEPARATE OPPOSING TRAFFIC.
- ⑧ SEE SHEET D FOR HOW TO ANCHOR BARRIER. SEE SHEET E FOR WHEN TO ANCHOR BARRIER.
- ⑨ 1" CHAMFER OPTIONAL.

f'c = 4,000 psi

CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

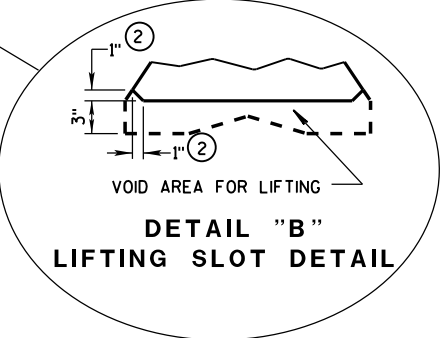
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



GENERAL NOTES

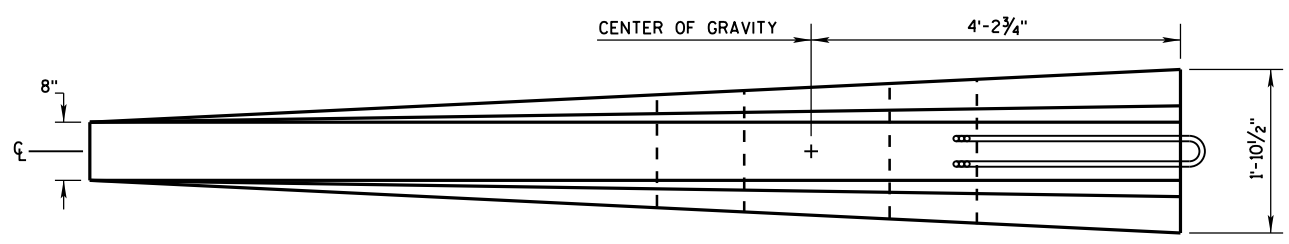
- ① MARK ONE END OF EACH BARRIER PERMANENTLY BY FORMING INTO THE BARRIER THE FOLLOWING INFORMATION:
 - a. TYPE WICBTP
 - b. MANUFACTURER
 - c. DATE MANUFACTURED (MONTH AND YEAR)
- ② 1" CHAMFER TO PREVENT SPALLING.
- ③ NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.

SIDE ELEVATION
 LOOP BAR ASSEMBLY INVERTED FOR OPPOSITE END.
 (FOR CONNECTION TO RIGHT END OF BARRIER)

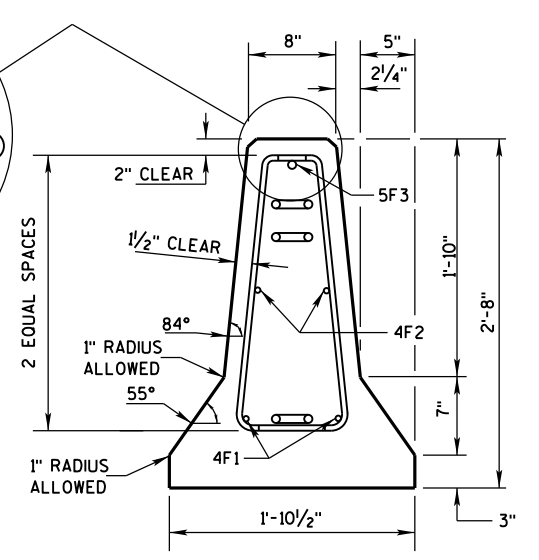
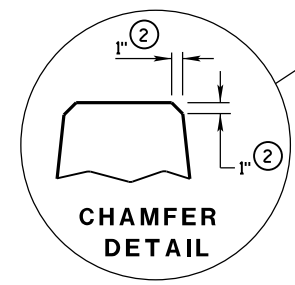


SIDE ELEVATION
 (FOR CONNECTION TO LEFT END OF BARRIER)

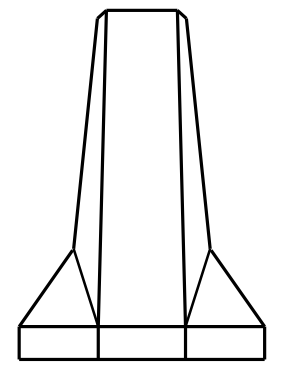
SEE DETAIL "C", BENT BAR DETAIL



PLAN VIEW

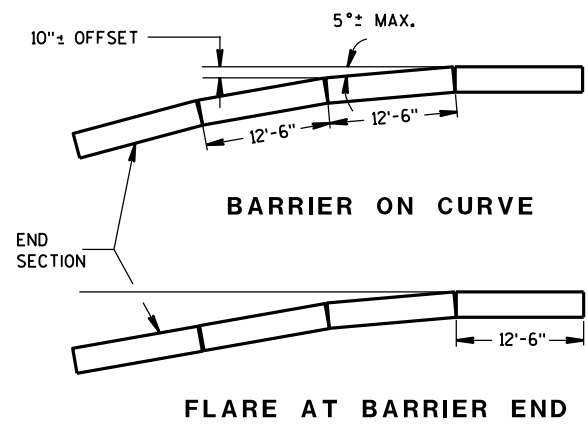


END SECTION



FRONT ELEVATION

DETAILS OF BARRIER TAPER SECTION



POSTED SPEED, (MPH)	FLARE RATE
40 OR LESS	6:1
45 OR GREATER	8:1

**CONCRETE BARRIER
 TEMPORARY PRECAST, 12'-6"**

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

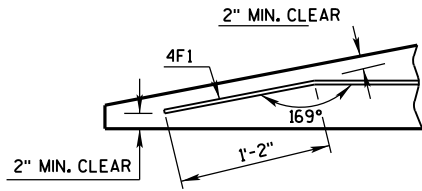
GENERAL NOTES

① NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.

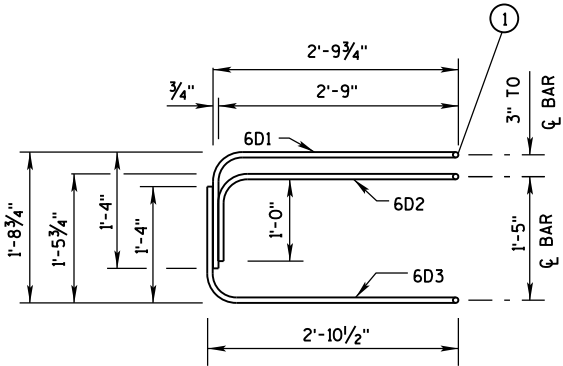
**BARRIER TAPER SECTION
BILL OF MATERIALS**
(PER 12'-6" BARRIER TAPER SECTION)

BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
4V1	4	2	1'-11"
4V2	4	2	2'-2"
4V3	4	2	2'-6"
4V4	4	2	2'-9"
4V5	4	2	3'-2"
4V6	4	2	3'-4"
4F1	4	2	12'-0"
4F2	4	2	7'-6"
5F3	5	1	11'-9"

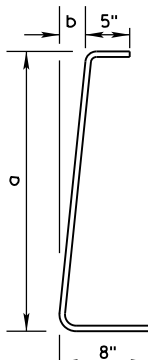
LOOP ASSEMBLY			
6D1	6	1	8'-5"
6D2	6	1	7'-7"
6D3	6	1	8'-6"



**DETAIL "C"
BENT BAR DETAIL**



**ELEVATION
LOOP BAR ASSEMBLY**



BAR	a	b
V1	10"	1"
V2	1'-1"	1/4"
V3	1'-5"	1 5/8"
V4	1'-8"	1 7/8"
V5	2'-0 1/2"	2 3/8"
V6	2'-3"	2 3/4"

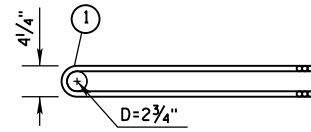
4V BARS
2 AT EACH SIZE REQUIRED
FOR STIRRUP ASSEMBLY

TAPER BARRIER SECTION

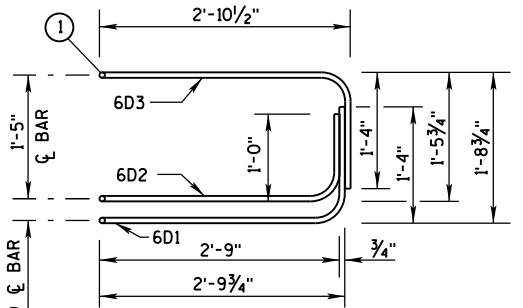
**BARRIER SECTION
BILL OF MATERIALS**
(PER 12'-6" BARRIER SECTION)

BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
4A1	4	12	6'-0"
6A2	6	6	2'-11"
5B1	5	3	12'-2"
4C1	4	2	12'-2"

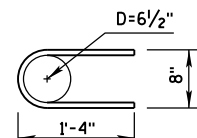
LOOP ASSEMBLY			
6D1	6	2	8'-5"
6D2	6	2	7'-7"
6D3	6	2	8'-6"



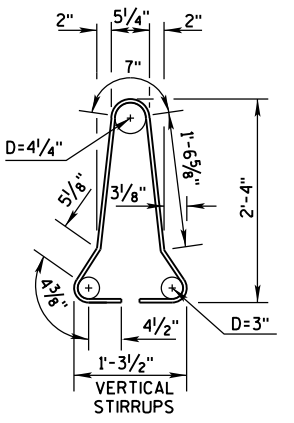
**PLAN VIEW
LOOP BAR ASSEMBLY**
(MARKED END SHOWN, INVERT FOR OTHER END)



ELEVATION VIEW



6A2

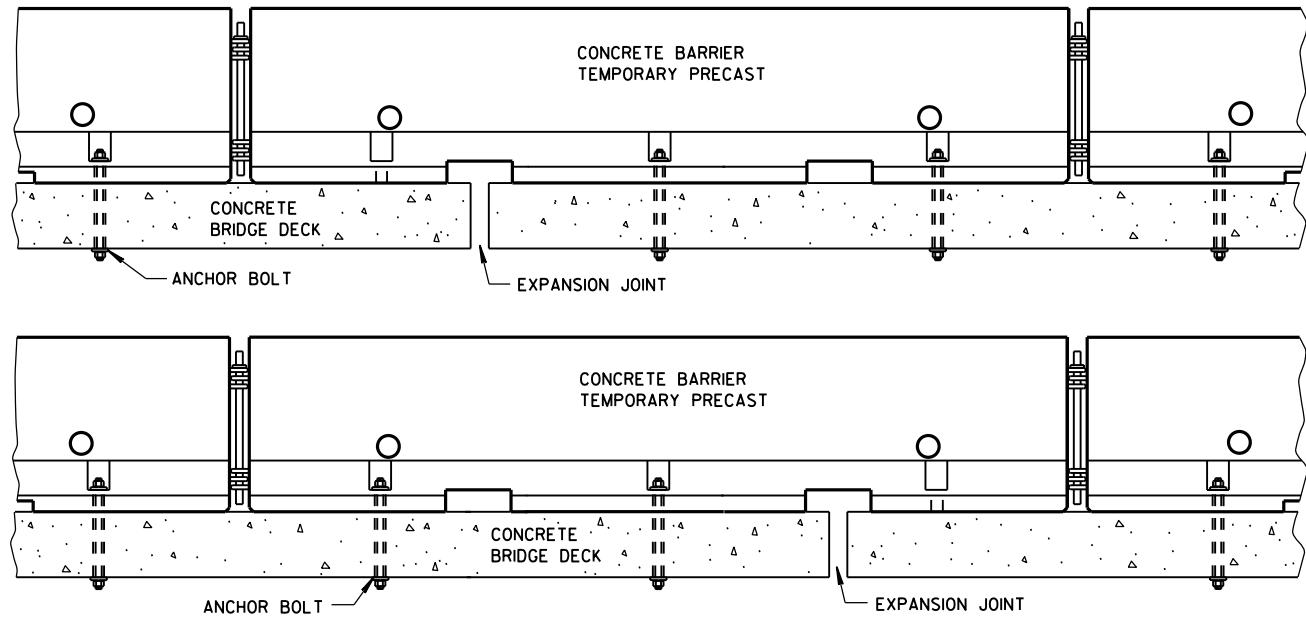


4A1

BARRIER SECTION

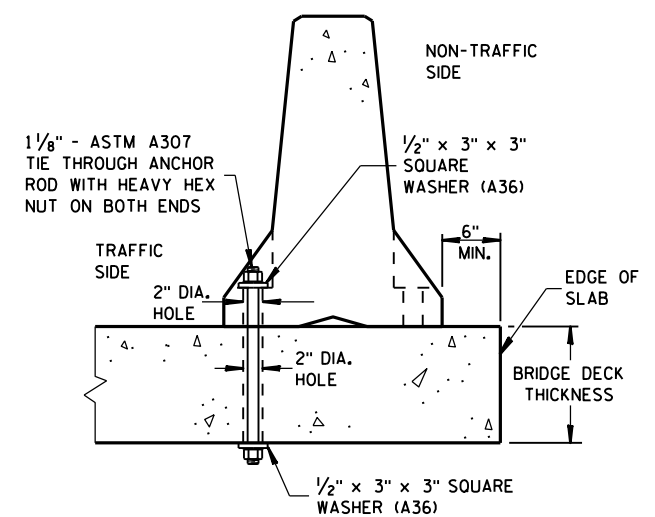
CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



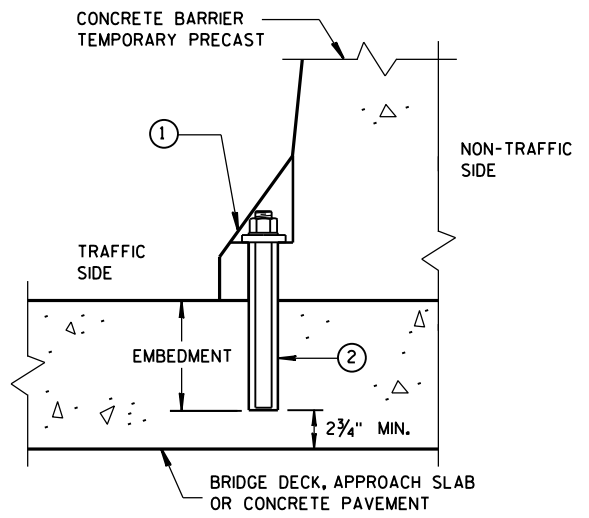
TREATMENT AT BRIDGE DECK EXPANSION JOINTS

(NO SINGLE CONCRETE BARRIER SECTION SHALL BE ANCHORED TO BOTH THE BRIDGE DECK AND THE APPROACH SLAB. ALL ANCHOR BOLT LOCATIONS SHALL BE ANCHORED TO THE DECK IN ACCORDANCE WITH THE DETAIL. NO MORE THAN ONE ANCHOR BOLT SHALL BE ELIMINATED FROM A BARRIER SECTION WHEN SPANNING AN EXPANSION JOINT.)



THROUGH BOLTED ANCHOR INSTALLATION ON BRIDGE DECK

(DO NOT USE ON CONCRETE BRIDGE DECK WITH ASPHALT OVERLAY)



REMOVABLE ADHESIVE ANCHOR INSTALLATION ON CONCRETE BRIDGE DECK, CONCRETE APPROACH SLAB, OR CONCRETE PAVEMENT

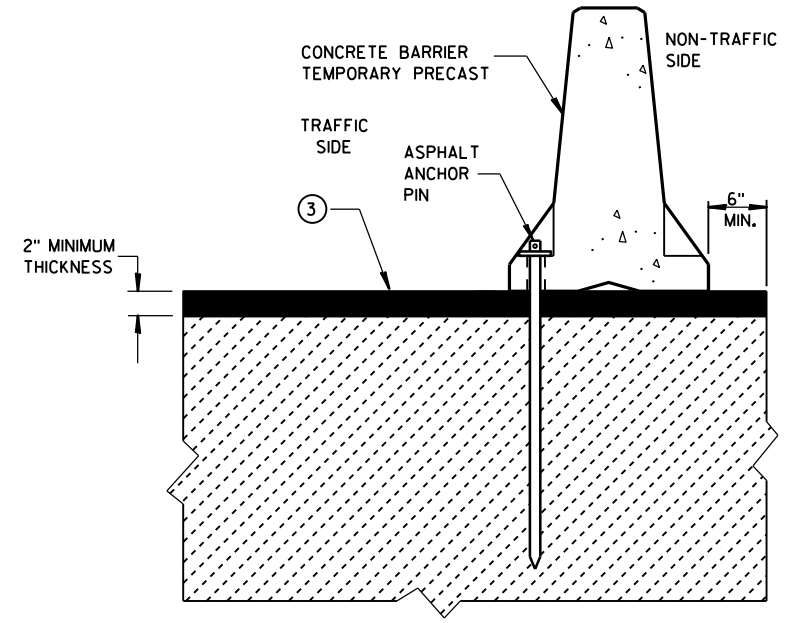
(DO NOT USE ON CONCRETE WITH AN ASPHALTIC OVERLAY)

GENERAL NOTES

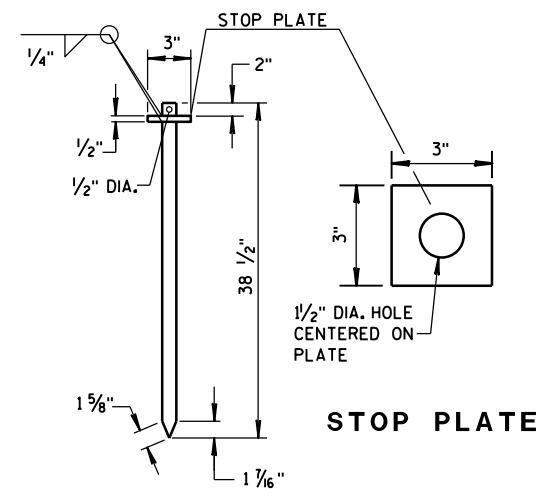
SEE SHEET E FOR WHEN TO ANCHOR. OTHER PARTS OF THE PLAN MAY SHOW ADDITIONAL LOCATIONS REQUIRING ANCHORING.

REMOVE ALL ANCHORS WHEN NO LONGER NEEDED. FILL CONCRETE PAVEMENTS, DECKS AND APPROACH SLABS WITH NON-SHRINK COMMERCIAL GROUT FROM THE APPROVED PRODUCT LIST. FILL ASPHALT PAVEMENTS WITH ASTM D6690 TYPE II RUBBERIZED CRACK FILLER.

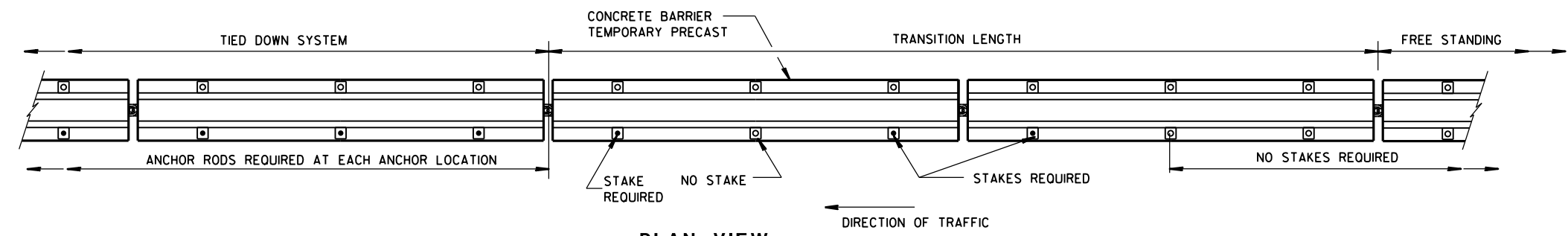
- ① 1/8" DIAMETER A307 THREADED ROD, 1/2" X 3" X 3" SQUARE PLATE WASHER WITH ASTM A36 STEEL, ASTM A563A HEAVY HEX NUT.
- ② ADHESIVE ANCHORS WITH A MINIMUM BOND STRENGTH OF 1,800 PSI AND 5/4" EMBEDMENT. SEE 603.2 AND 603.3.1.2 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.
- ③ ASPHALT SURFACE SHOWN. CONTRACTOR MAY DRILL THROUGH CONCRETE PAVEMENT AND THEN DRIVE ASPHALT ANCHOR PIN.



STAKE DOWN INSTALLATION FOR ASPHALTIC SURFACE



ASPHALT ANCHOR PIN (ASTM A36 STEEL)



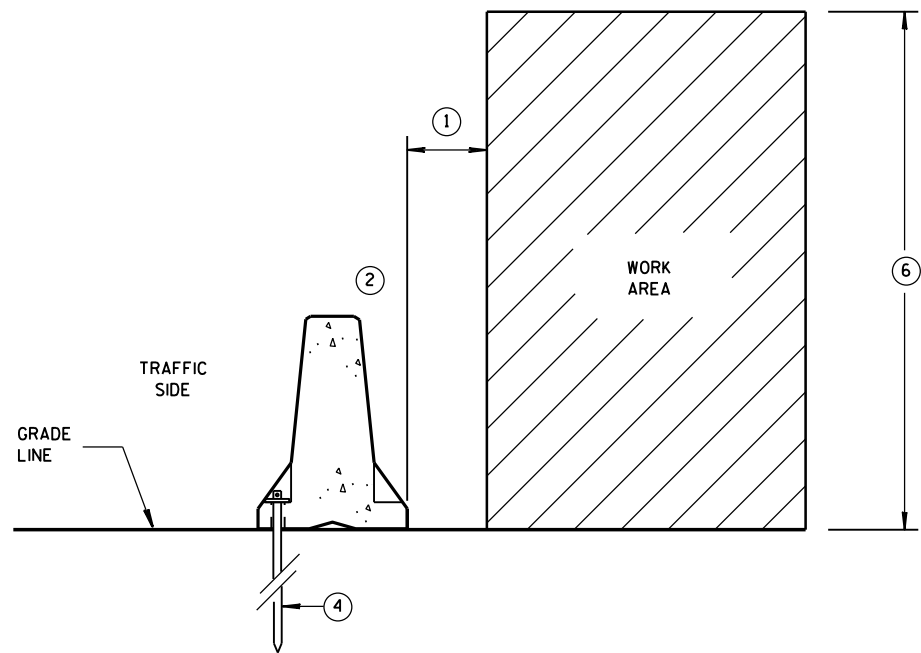
FREE STANDING TRANSITION TO TIED-DOWN SYSTEM

(PLACE TRANSITION IN A TANGENT SECTION OF BARRIER PARALLEL TO THE ROADWAY. IF TRANSITION OCCURS ON STRUCTURAL SLAB, ANCHOR AS SHOWN.)

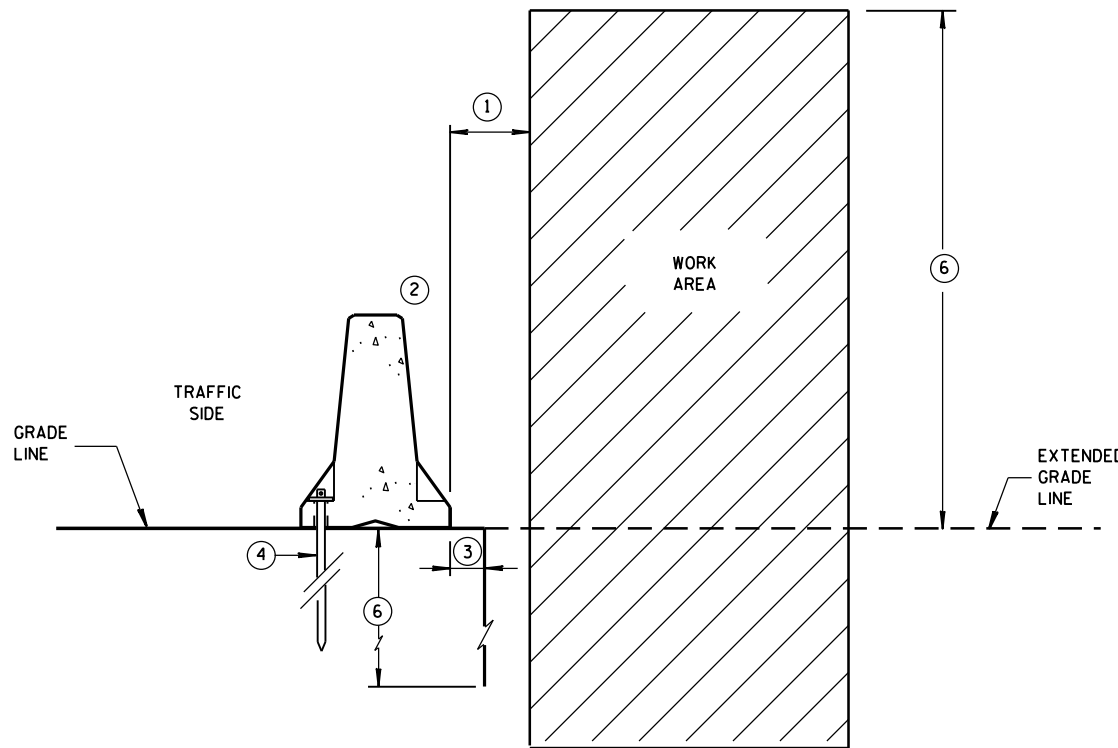
CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

- ① WHEN OBJECTS EXTEND ABOVE THE GRADE, A MINIMUM OF 1 FOOT IS REQUIRED FROM BACK OF BARRIER TO OBJECT. SEE OTHER DETAILS FOR FOR THE MINIMUM OFFSET FROM BACK OF BARRIER TO SLOPES OR VERTICAL DROPS.
- ② OBJECTS ARE NOT TO BE PLACED ON, MOUNTED TO, OR LEANED AGAINST THE BARRIER WITHOUT PERMISSION OF THE PROJECT ENGINEER.
- ③ SEE OTHER DETAIL ON SHEET "D" FOR SPACE REQUIREMENTS.
- ④ SEE BOLT THROUGH DECK, REMOVABLE ADHESIVE ANCHOR, OR A STAKE DOWN FOR ASPHALTIC SURFACE TREATMENT DETAILS. ASPHALTIC ANCHOR SHOWN.
- ⑤ DEPTH OF 3 FEET OR MORE.
- ⑥ Y = 6'-6".

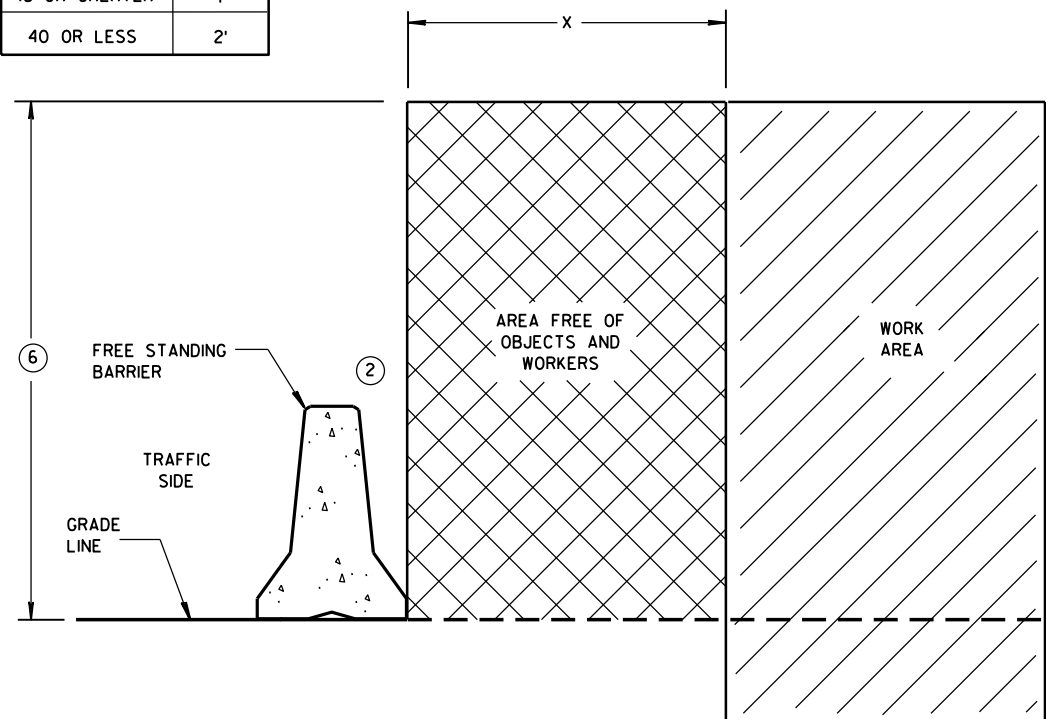


ANCHORED BARRIER SPACE REQUIREMENTS FOR HAZARDS EXTENDED ABOVE THE GRADE LINE

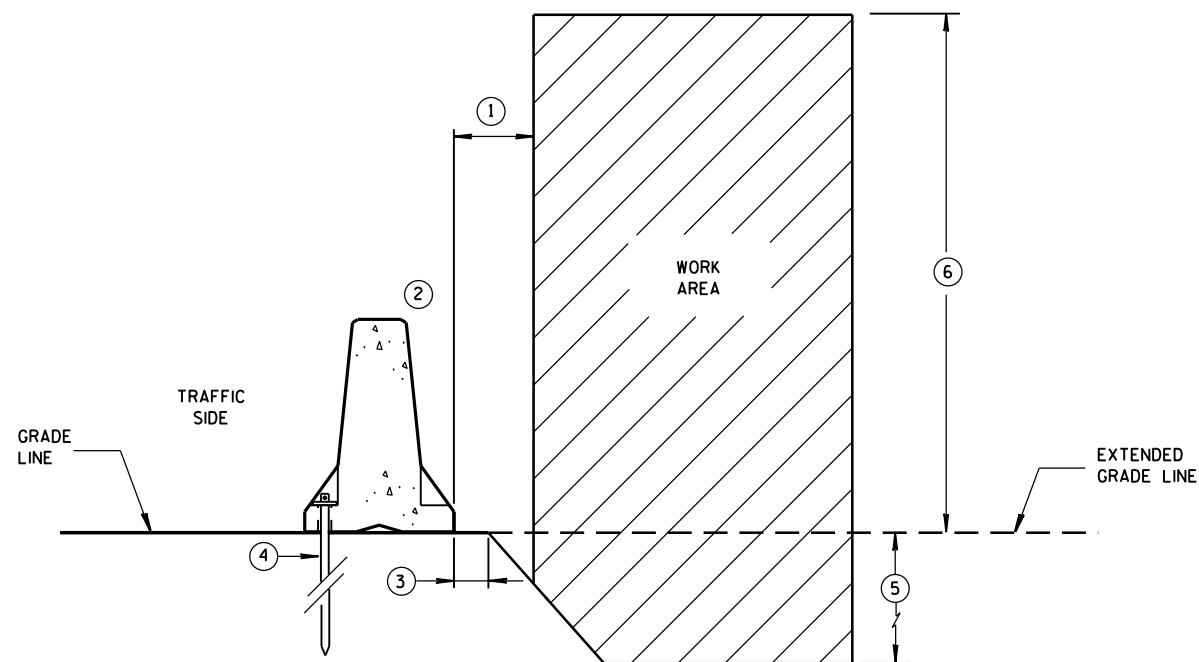


ANCHORED BARRIER SPACE REQUIREMENTS ON VERTICAL DROP OFFS

POSTED SPEED MPH	X
45 OR GREATER	4'
40 OR LESS	2'



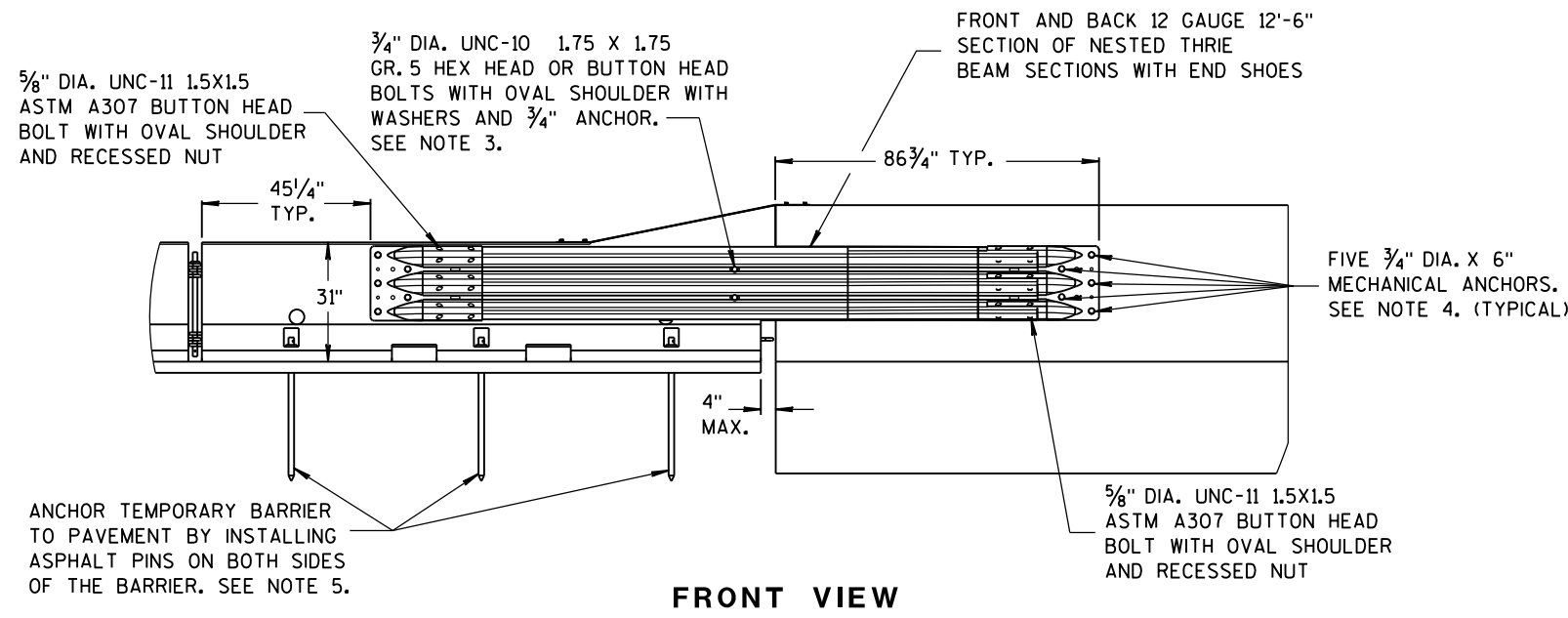
FREE STANDING BARRIER SPACE REQUIREMENTS



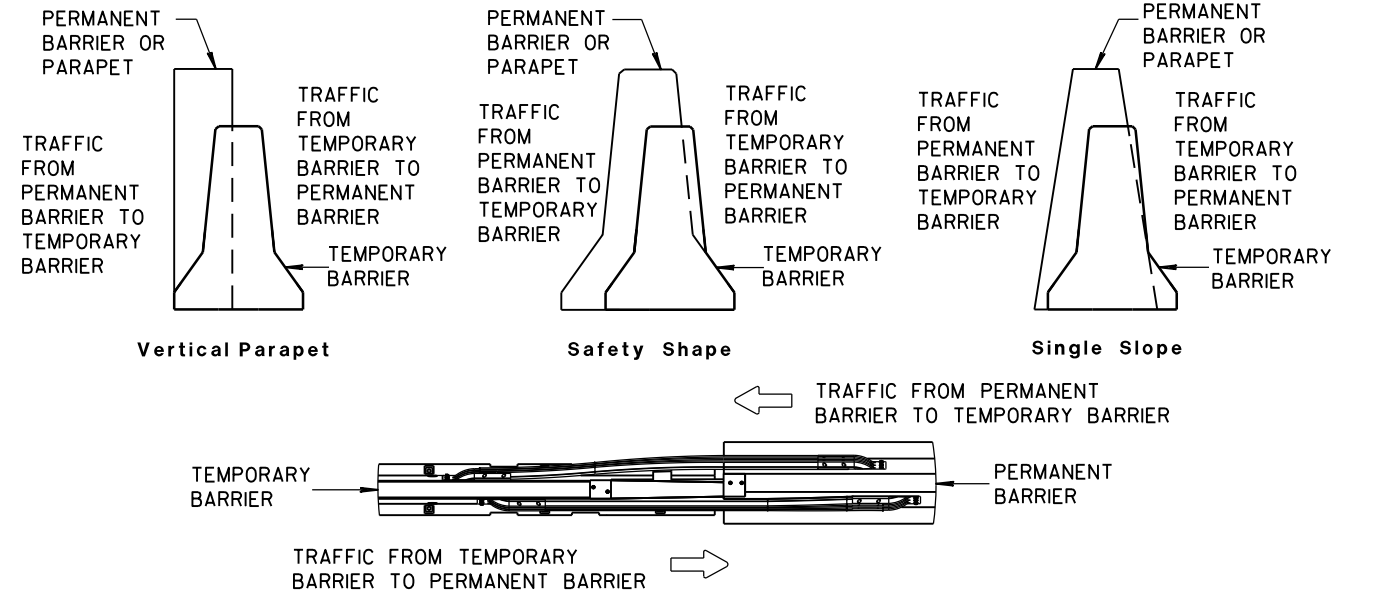
ANCHORED BARRIER SPACE REQUIREMENTS ON SLOPES

**CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

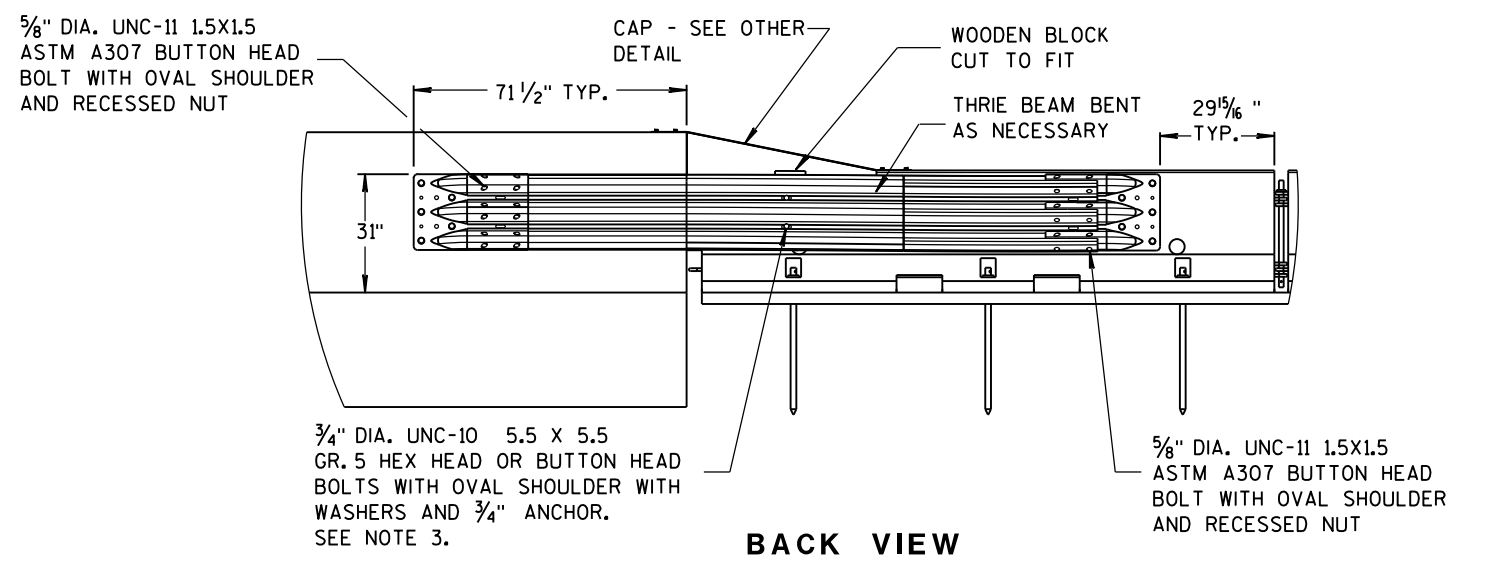


FRONT VIEW

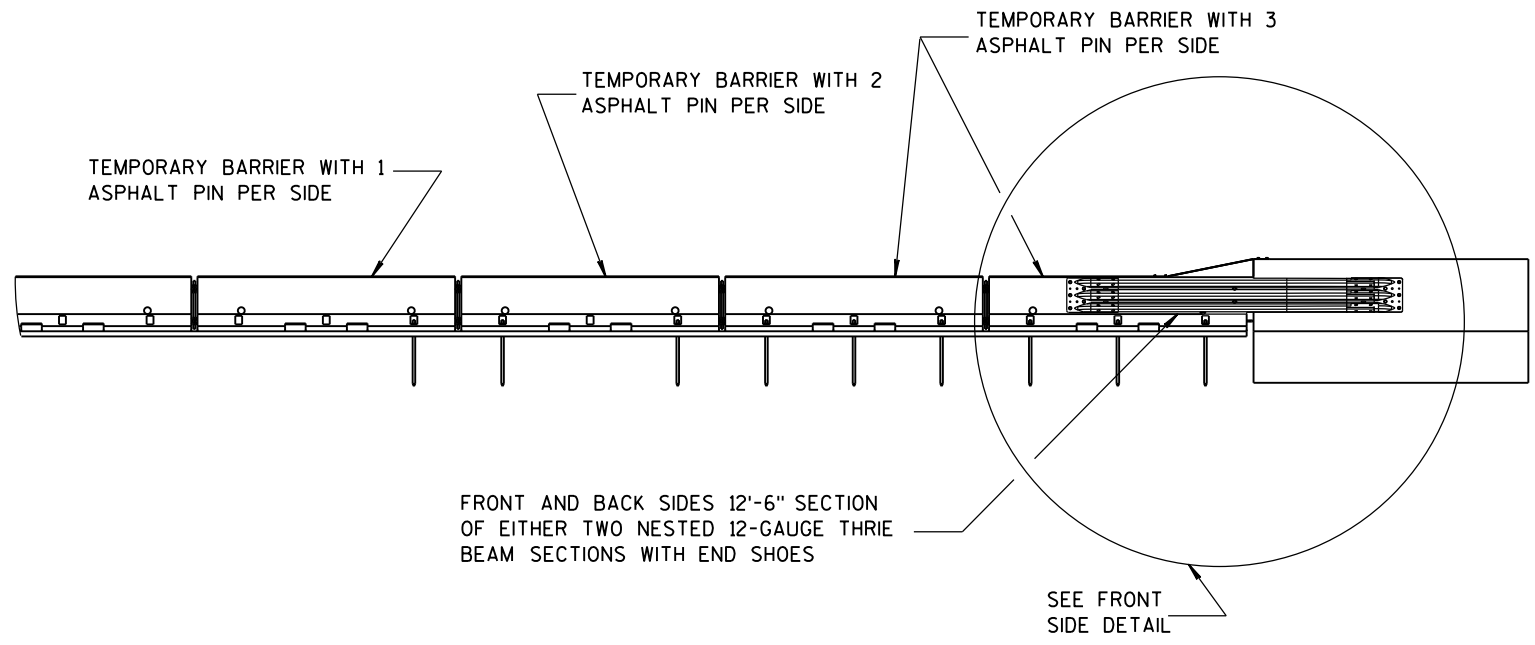


TEMPORARY BARRIER PLACEMENT FOR BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM

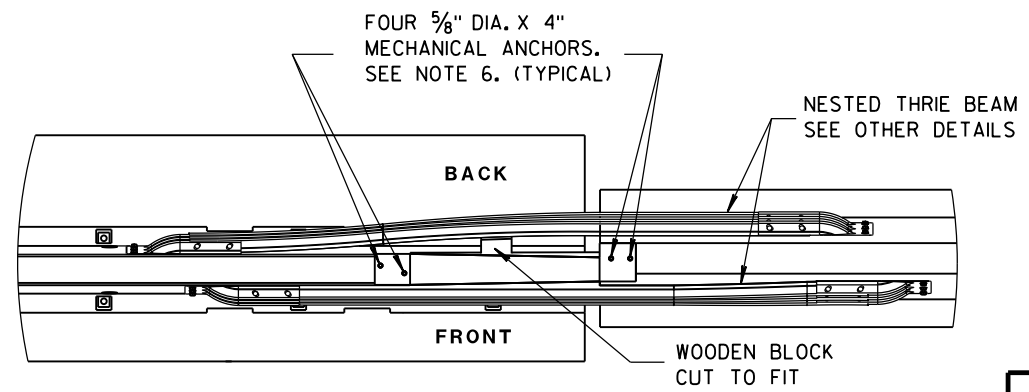
- NOTES**
1. CAP END PLATE PLACED FLUSH WITH UPSTREAM END OF PERMANENT BARRIER OR PARAPET.
 2. THRIE BEAM PIECES ARE OFFSET 15 1/4" TO PREVENT INTERFERENCE FROM THE ANCHORS ON OPPOSING SIDES.
 3. MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 9.48 KIPS AND ULTIMATE SHEAR LOAD 10.48 KIPS.
 4. MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 17.9 KIPS AND ULTIMATE SHEAR LOAD 21.96 KIPS.
 5. MAY BE USED ON CONCRETE OR ASPHALT PAVEMENTS. ASPHALT OPTION SHOWN. FOR CONCRETE OPTION SEE OTHER DETAILS.
 6. MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 12.14 KIPS AND ULTIMATE SHEAR LOAD 17.5 KIPS.



BACK VIEW



FRONT VIEW

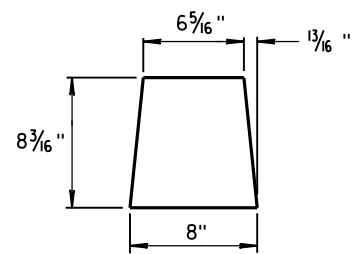


PLAN VIEW

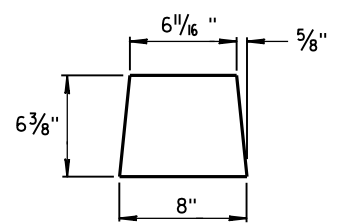
BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM

**CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"**

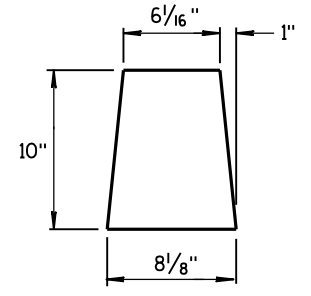
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



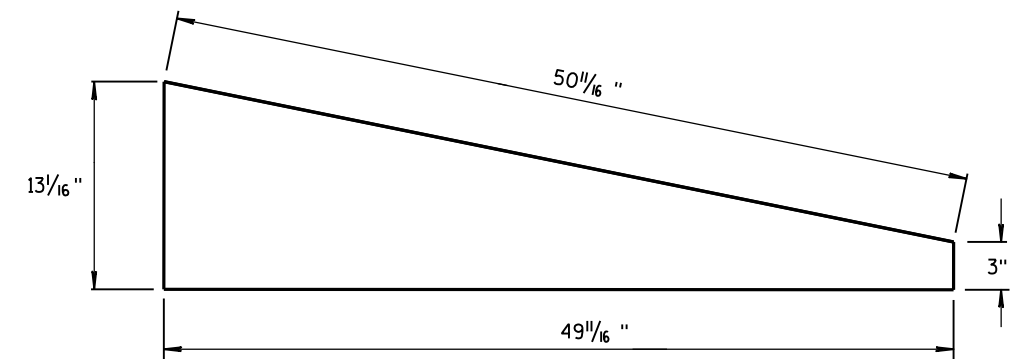
GUSSET 1



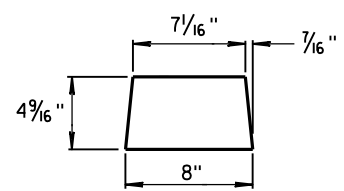
GUSSET 2



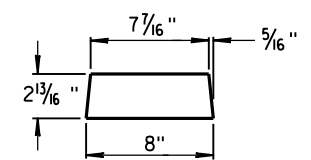
END PLATE



SIDE PLATE

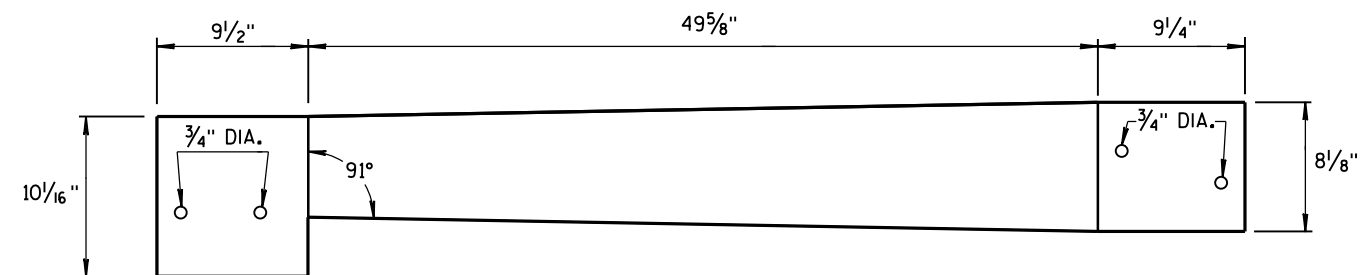


GUSSET 3

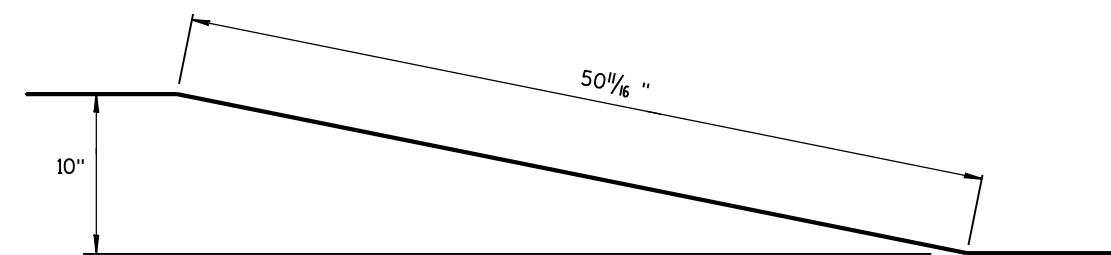


GUSSET 4

GUSSETS

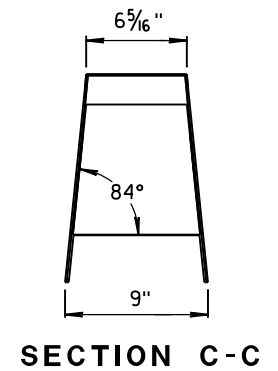
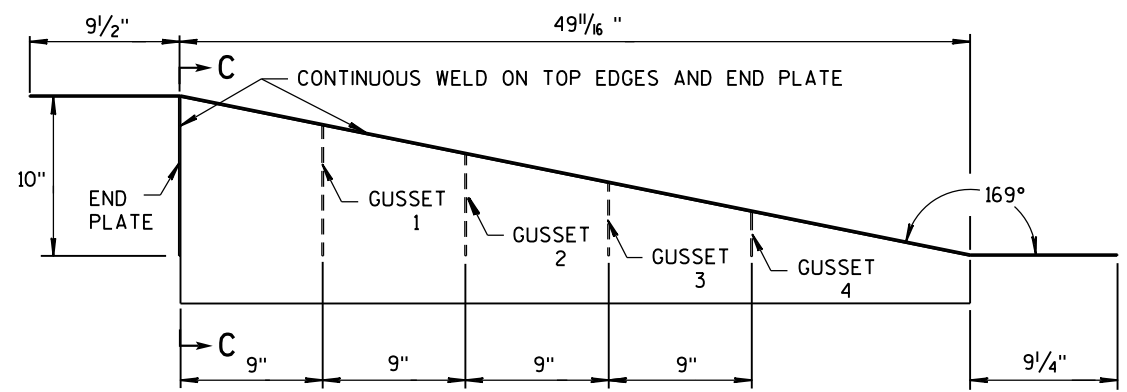
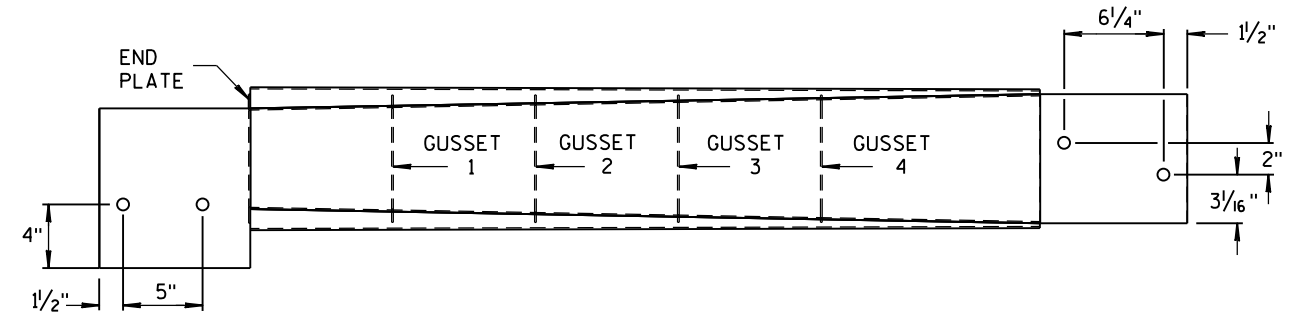


TOP PLATE



SIDE, TOP AND END PLATES FOR CAP FROM TEMPORARY CONCRETE BARRIER TO 42" PERMANENT CONCRETE BARRIER

SIDE PLATES, TOP PLATE, END PLATE AND GUSSETS ARE 12 GAUGE ASTM A36 GALVANIZED STEEL.



SECTION C-C

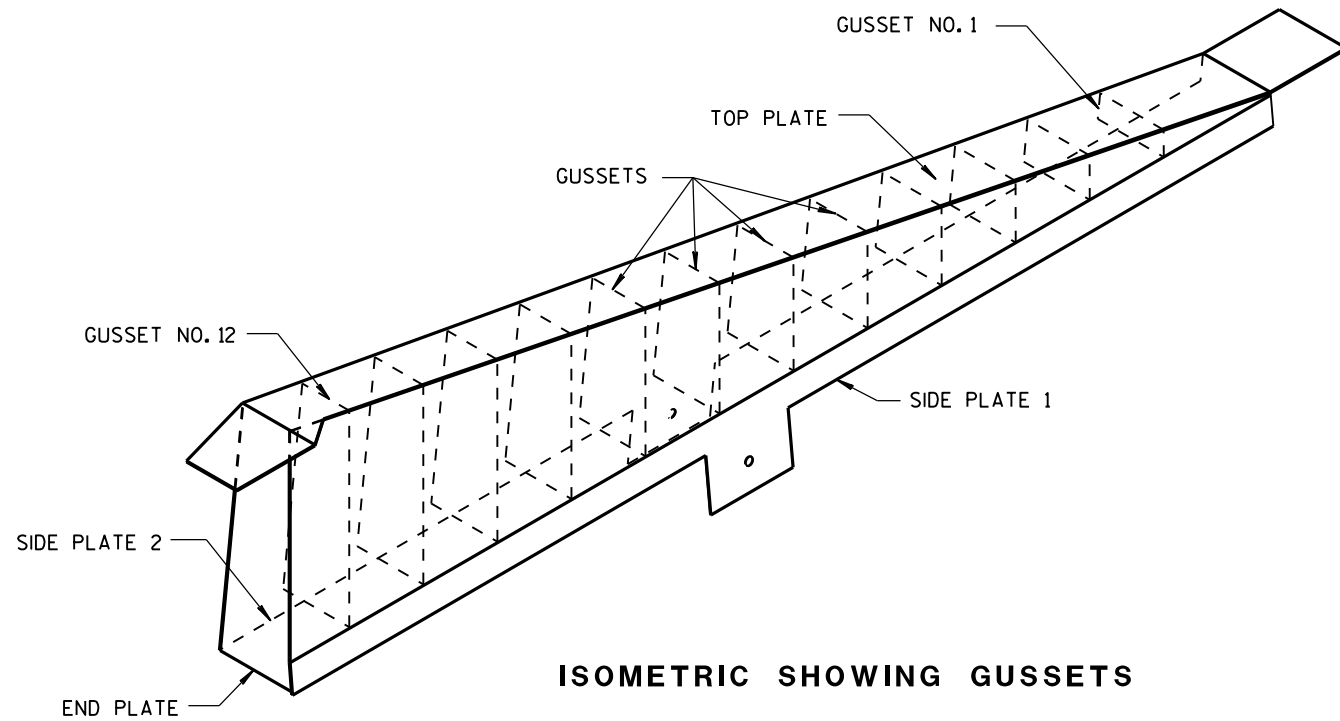
NOTES

- FOUR GUSSETS AND END PLATE ARE STITCH WELDED ON THREE SIDES.
- TWO TRIANGULAR SIDE PLATES ARE STITCH WELDED TO TOP PLATE, END PLATE, AND GUSSETS.

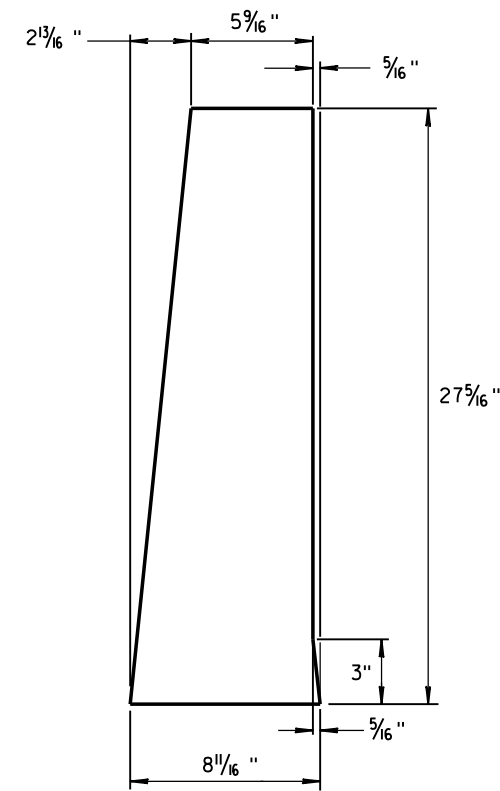
CAP DETAILS FOR TEMPORARY CONCRETE BARRIER TO 42" PERMANENT CONCRETE BARRIER

CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

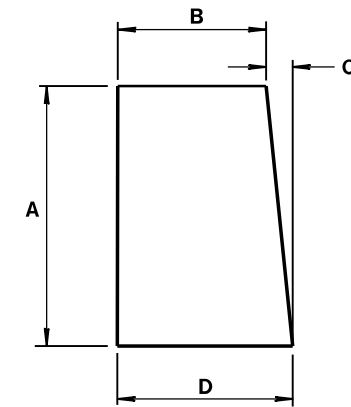


ISOMETRIC SHOWING GUSSETS



END PLATE

1/8" STEEL PLATE



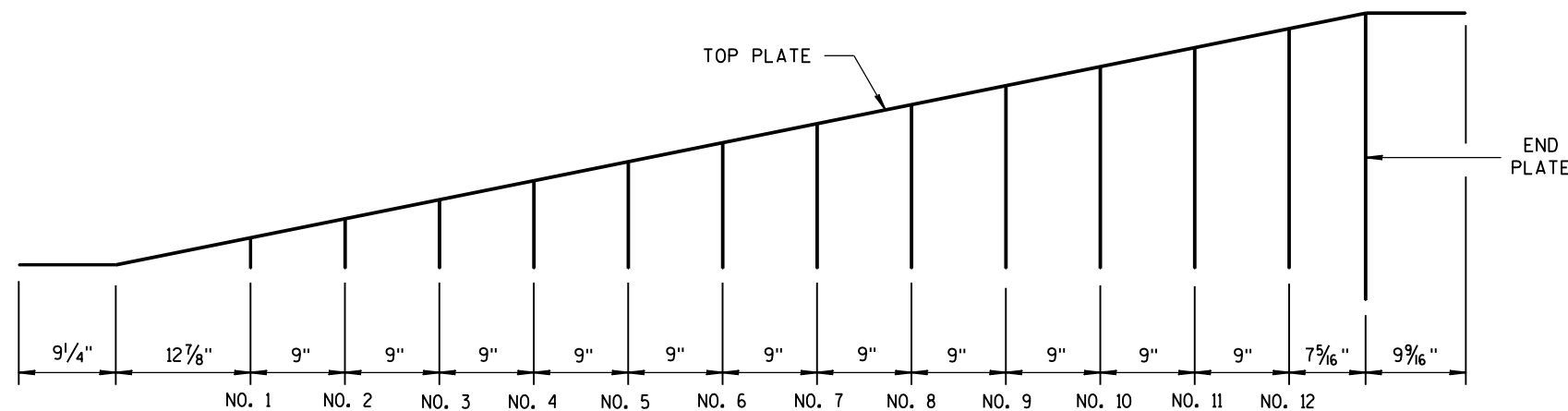
GUSSETS 1 - 12

ALL GUSSETS 1/8" STEEL PLATE

GUSSET DIMENSIONS				
GUSSET NO.	A	B	C	D
1	2 7/8"	7 3/4"	1/4"	8
2	4 1/16 "	7 7/16 "	1/2"	8
3	6 1/2"	7 3/8 "	11/16 "	8 1/16 "
4	8 5/16 "	7 3/16 "	7/8 "	8 1/16 "
5	10 1/8 "	7"	1 1/16 "	8 1/16 "
6	11 5/16 "	6 13/16 "	1 1/4"	8 1/16 "
7	13 3/4"	6 5/8 "	1 7/16 "	8 1/16 "
8	15 3/16 "	6 7/16 "	1 9/16 "	8 1/16 "
9	17 3/8 "	6 1/4"	1 13/16 "	8 1/16 "
10	19 3/16 "	6 1/16 "	1 15/16 "	8 1/16 "
11	21"	5 7/8 "	2 3/16 "	8 1/16 "
12	22 13/16 "	5 1/16 "	2 5/16 "	8 1/16 "

SIDE PLATES, TOP PLATE, END PLATE AND GUSSETS ARE 12 GAUGE ASTM A36 STEEL AND GALVANIZED.

GUSSETS AND END PLATE ARE STITCH WELDED ON 3 SIDES. TWO TRIANGULAR SIDE PLATES ARE STITCH WELDED TO TOP PLATE, END PLATE AND GUSSETS.

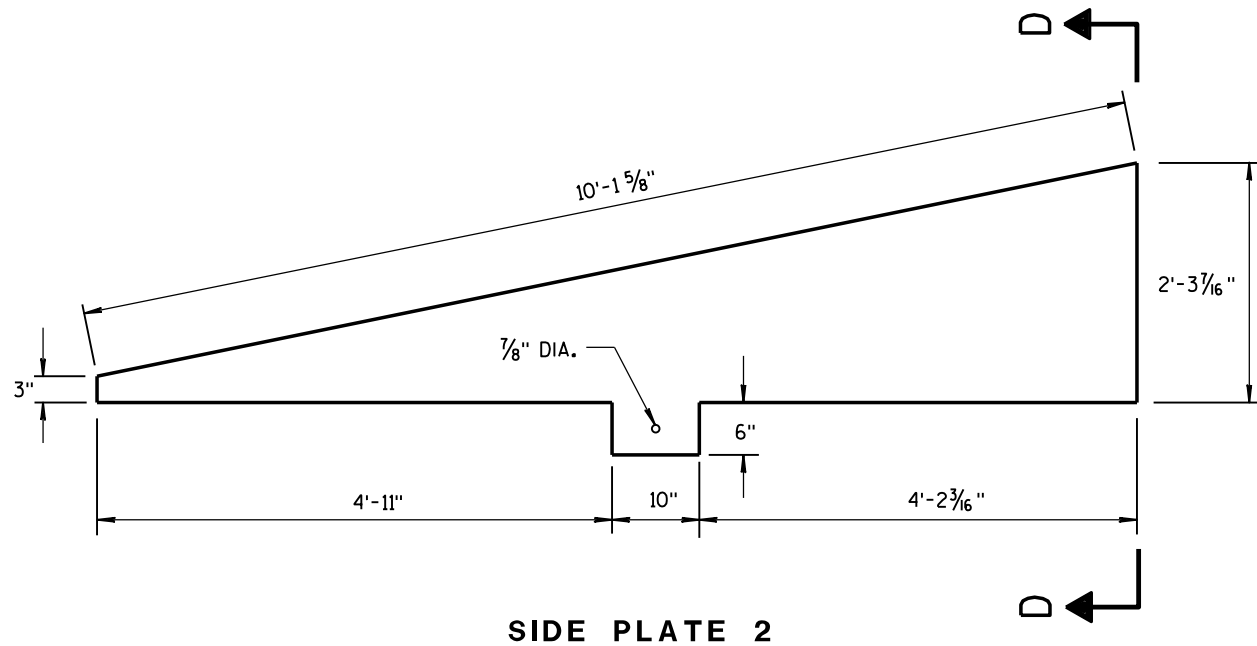


GUSSET LOCATION

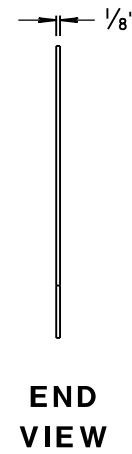
CAP DETAILS FOR TEMPORARY CONCRETE BARRIER TO 56" PERMANENT CONCRETE BARRIER

CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

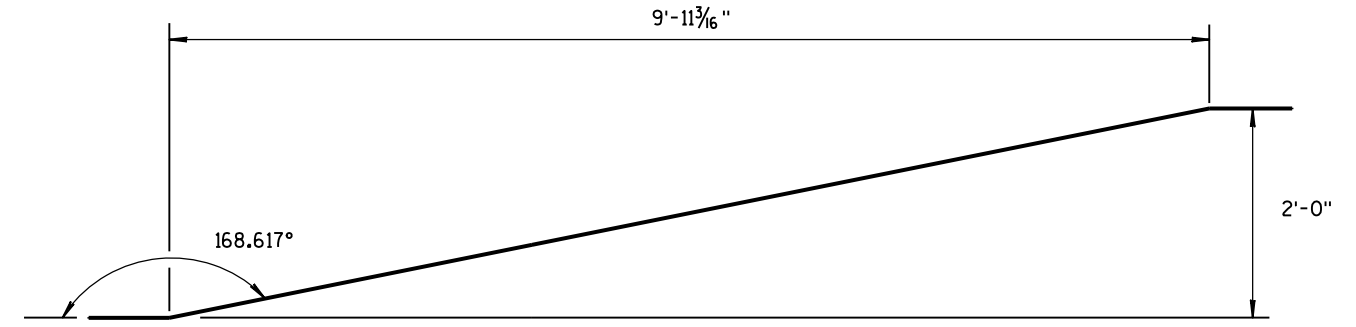
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



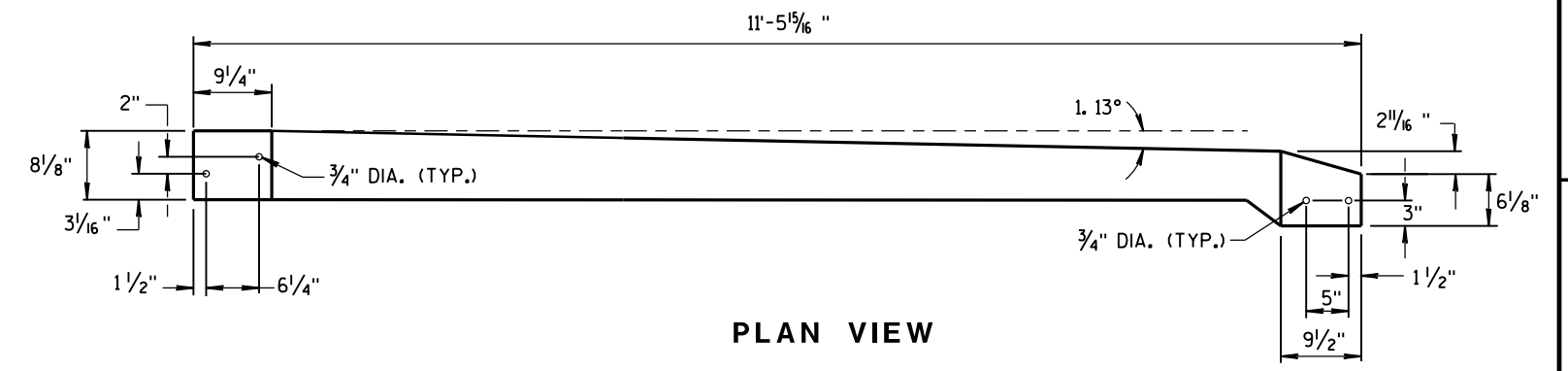
SIDE PLATE 2



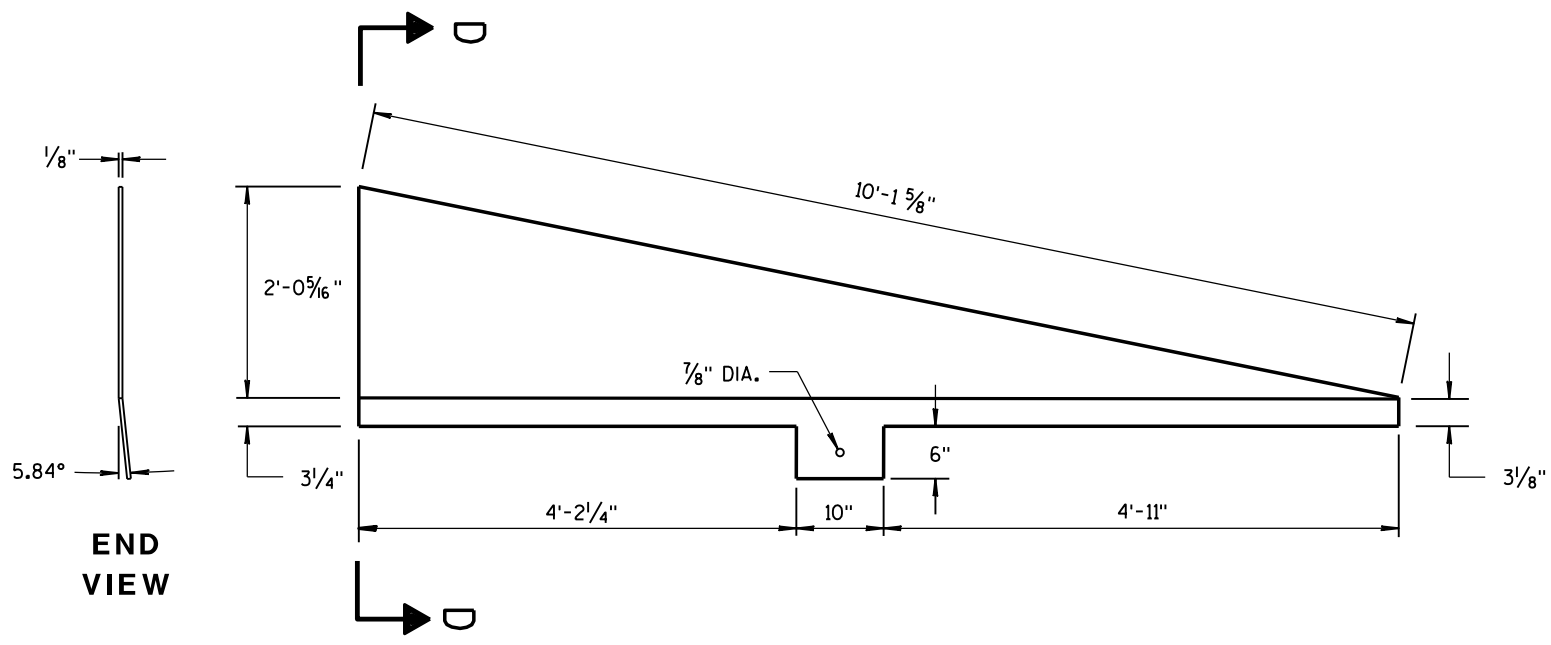
END VIEW



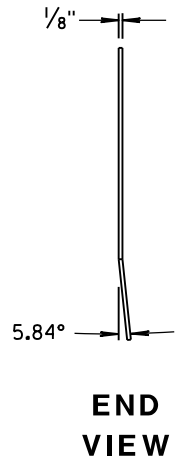
SIDE VIEW
TOP PLATE



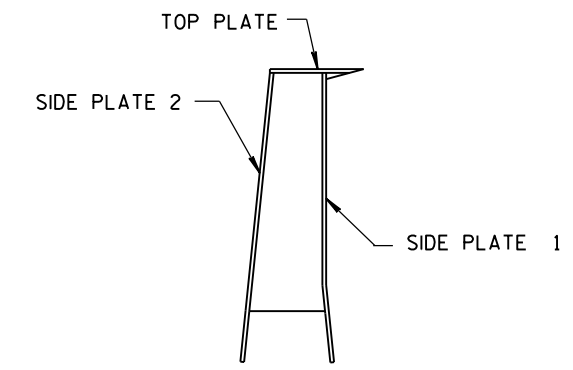
PLAN VIEW
TOP PLATE



SIDE PLATE 1



END VIEW



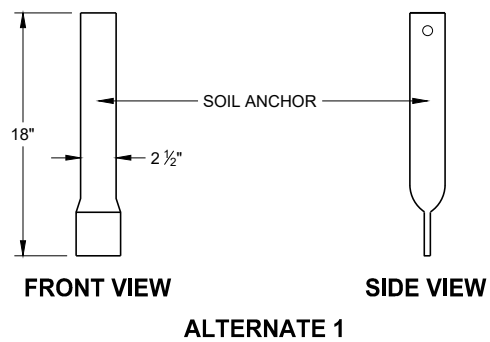
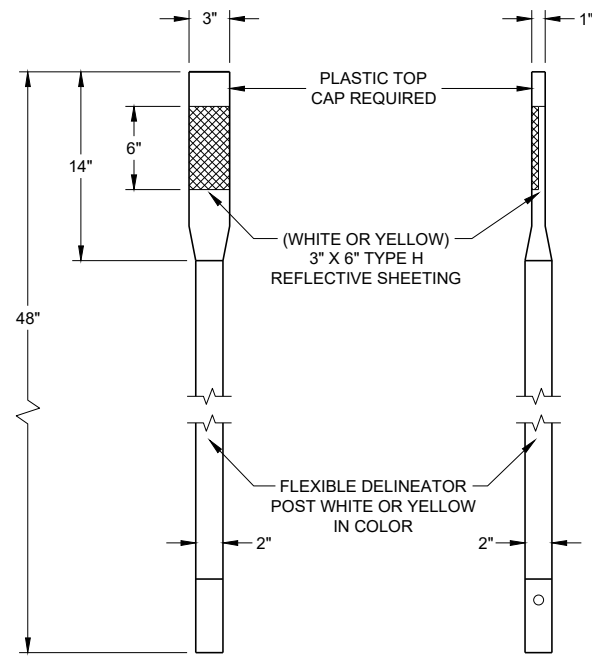
SECTION D-D

CAP DETAILS FOR TEMPORARY CONCRETE BARRIER TO 56" PERMANENT CONCRETE BARRIER

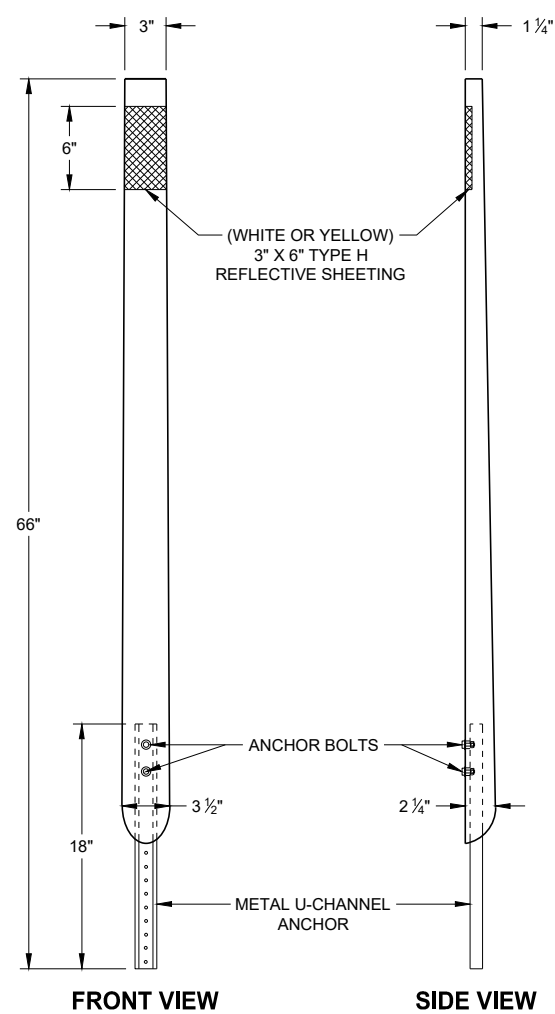
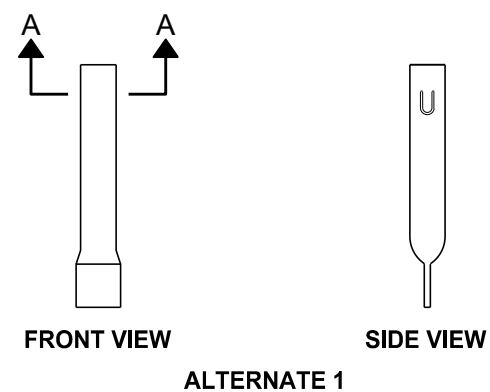
**CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

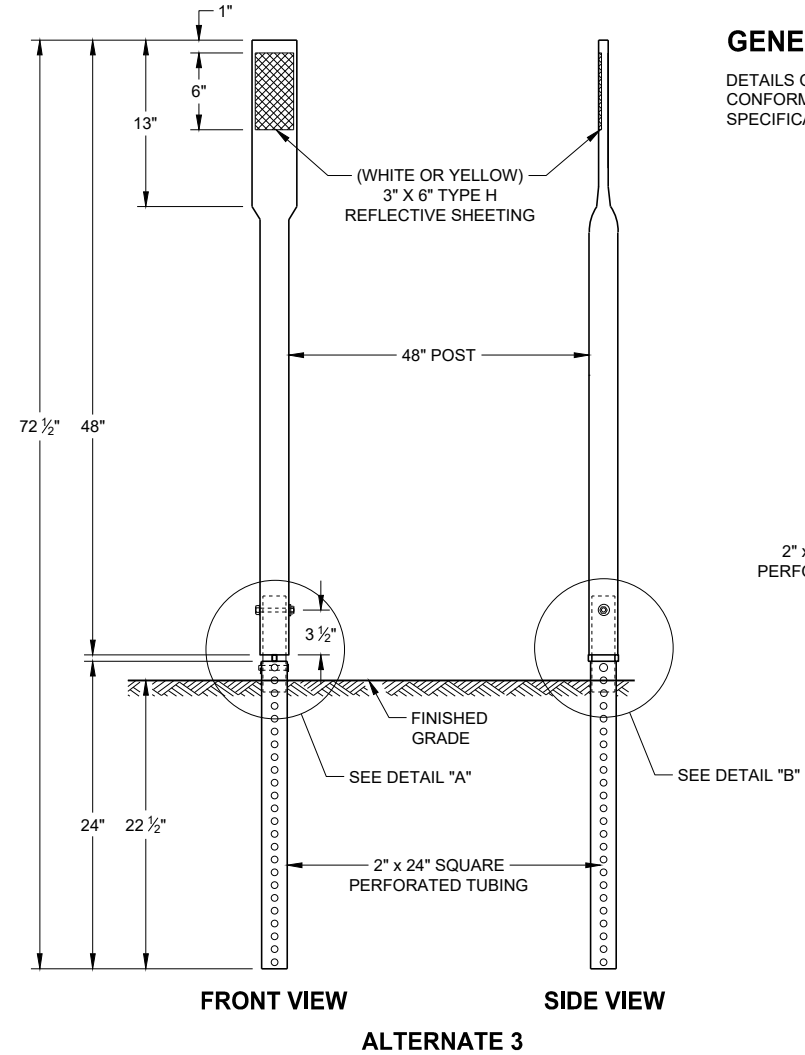
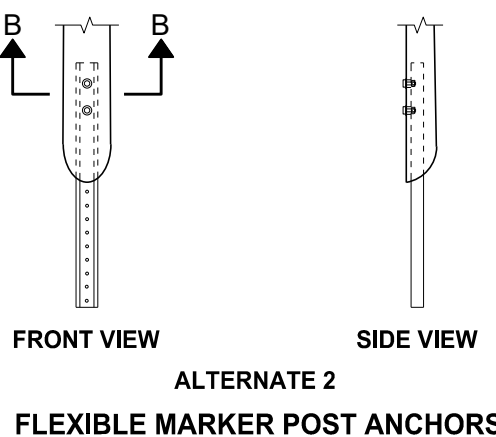
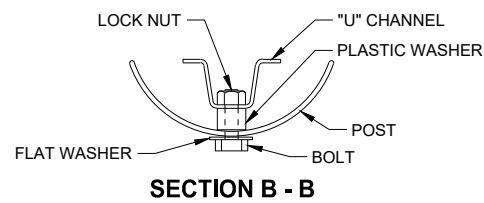
APPROVED
June 2017 /S/ Rodney Taylor
DATE ROADWAY STANDARD DEVELOPMENT
FHWA UNIT SUPERVISOR



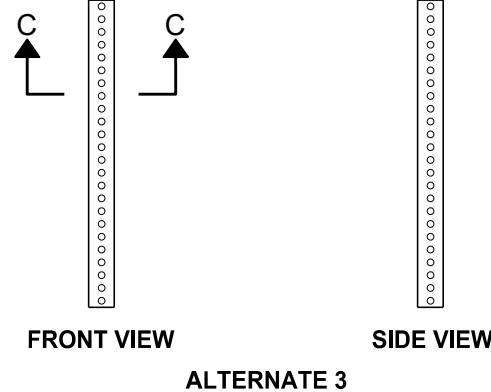
SECTION A - A



FLEXIBLE DELINEATOR POSTS

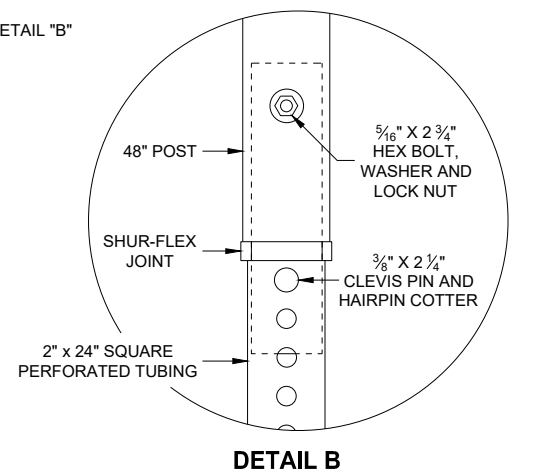
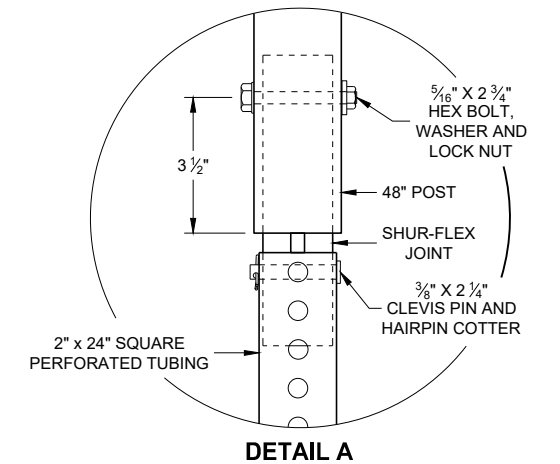


SECTION C - C



GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



REFLECTOR SPACING TABLE

REFLECTOR SPACING	LOCATION
* 100' C-C	RAMPS
400' C-C	MAINLINE

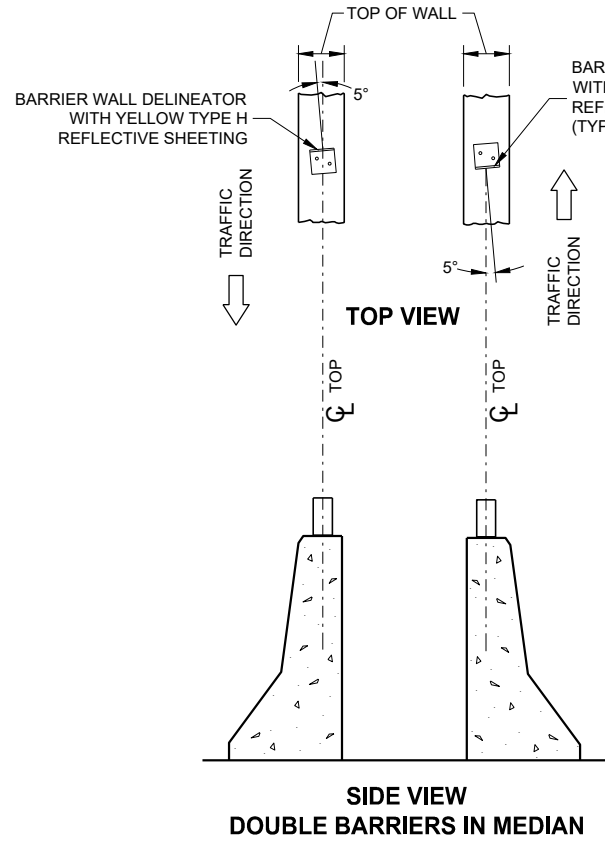
* START AT BEGINNING OF RAMP TAPER AND END AT END OF RAMP TAPER

FLEXIBLE DELINEATOR POST

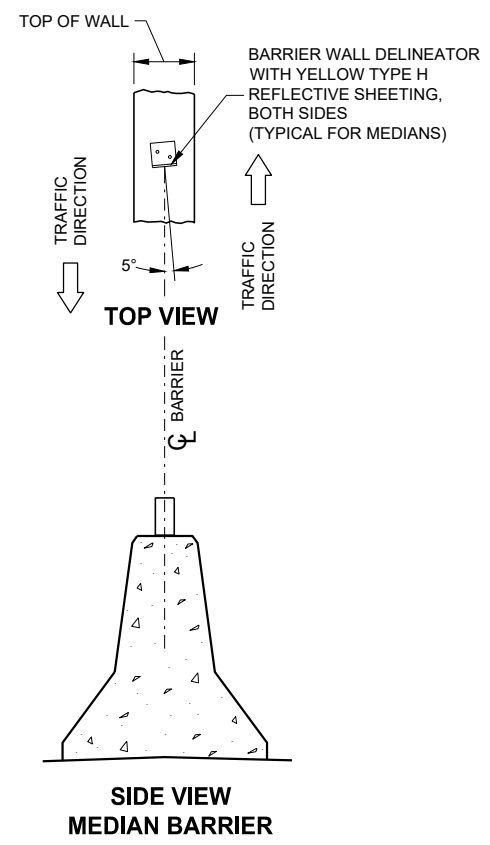
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

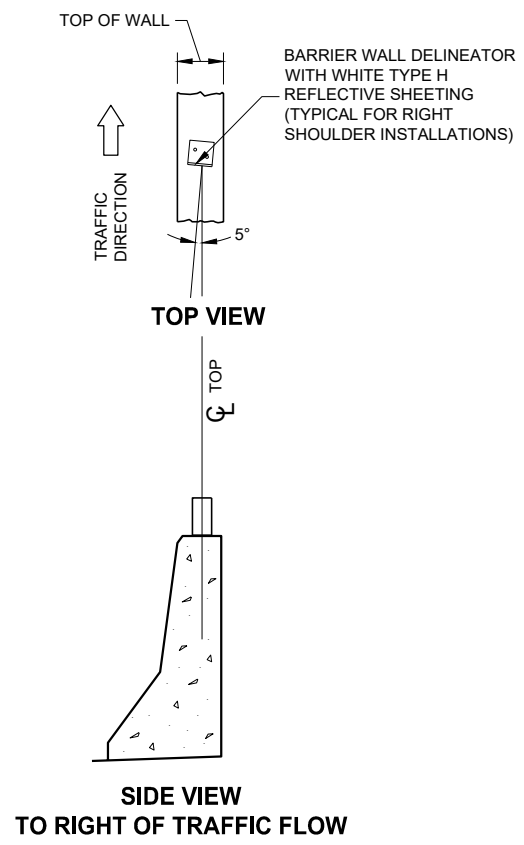
FHWA



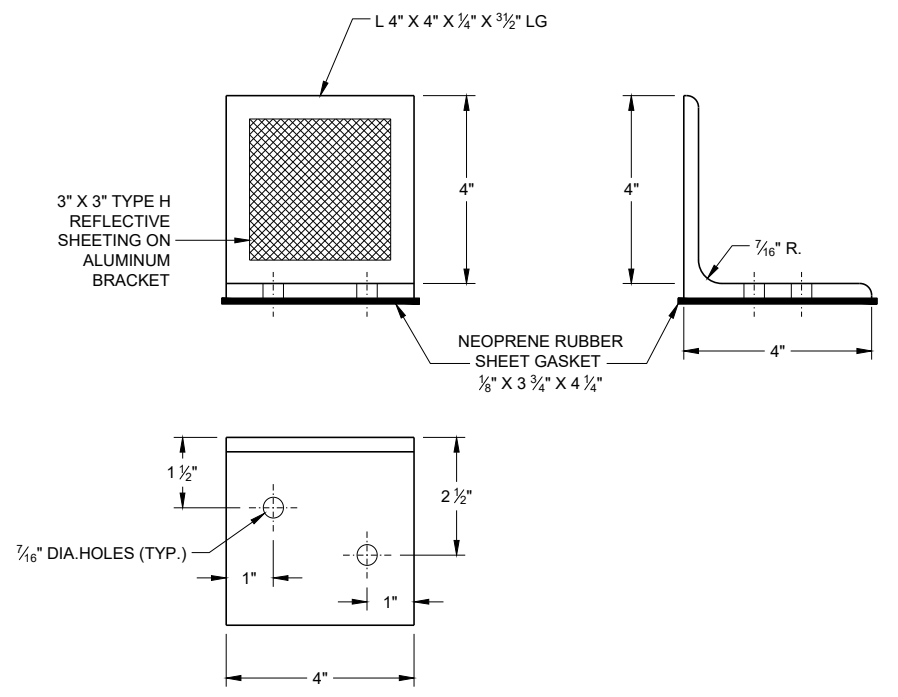
**SIDE VIEW
DOUBLE BARRIERS IN MEDIAN**



**SIDE VIEW
MEDIAN BARRIER**

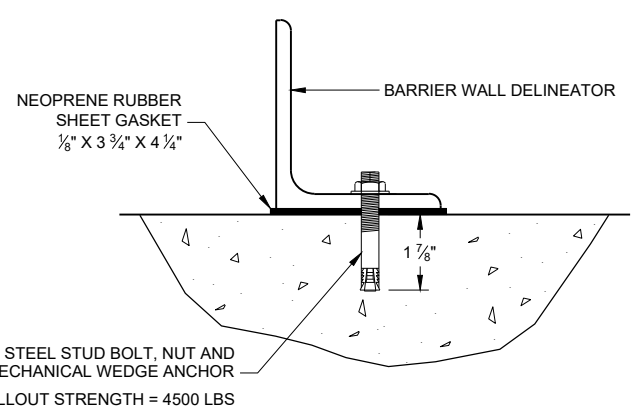


**SIDE VIEW
TO RIGHT OF TRAFFIC FLOW**



BARRIER WALL DELINEATOR

LOCATION AND AIMING DETAILS FOR BARRIER WALL DELINEATOR MOUNTED ON CONCRETE BARRIERS



2 - 3/8" DIA. STAINLESS STEEL STUD BOLT, NUT AND WASHER WITH MECHANICAL WEDGE ANCHOR
MIN. COMBINED PULLOUT STRENGTH = 4500 LBS

BARRIER WALL DELINEATOR MOUNTING DETAIL

REFLECTOR SPACING TABLE

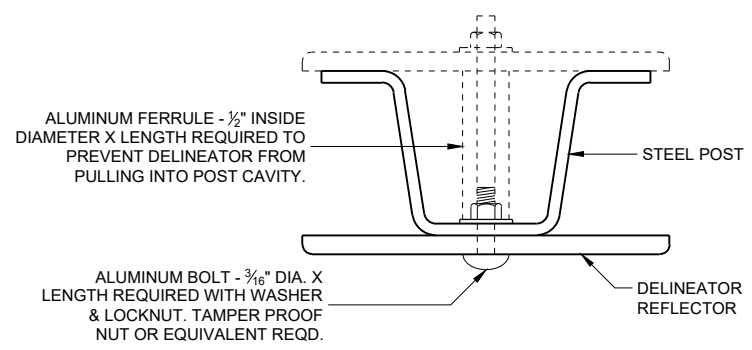
REFLECTOR SPACING	MINIMUM NUMBER OF REFLECTORS
100' C-C	3

BARRIER WALL DELINEATOR WITH REFLECTIVE SHEETING

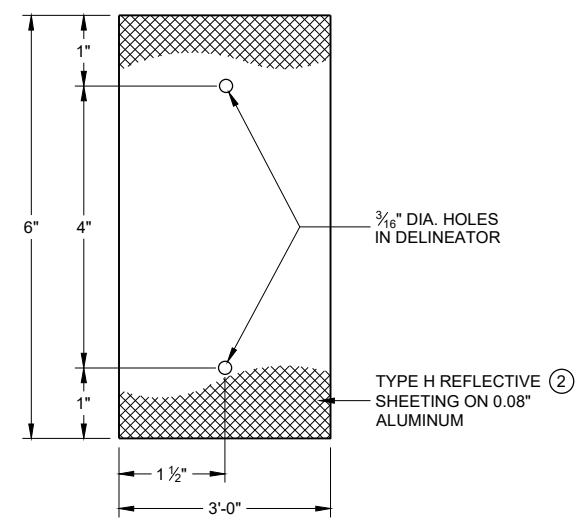
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APPROVED
August 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

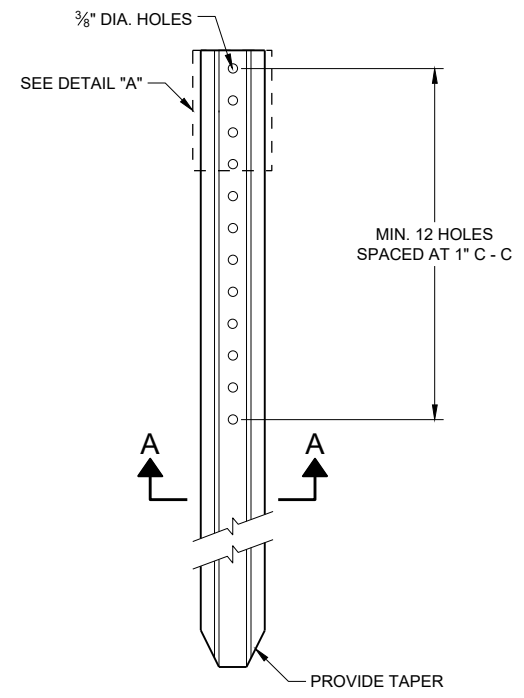
FHWA



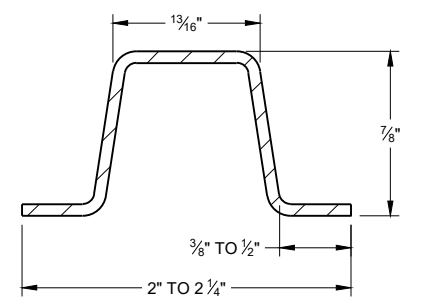
MOUNTING DETAIL FOR DELINEATOR REFLECTOR



DETAIL "A" 3" X 6" DELINEATOR REFLECTOR



DELINEATOR POST



SECTION A - A
WEIGHT 1.12 LBS PER FT. \ 0.1 LB.

REFLECTOR SPACING TABLE

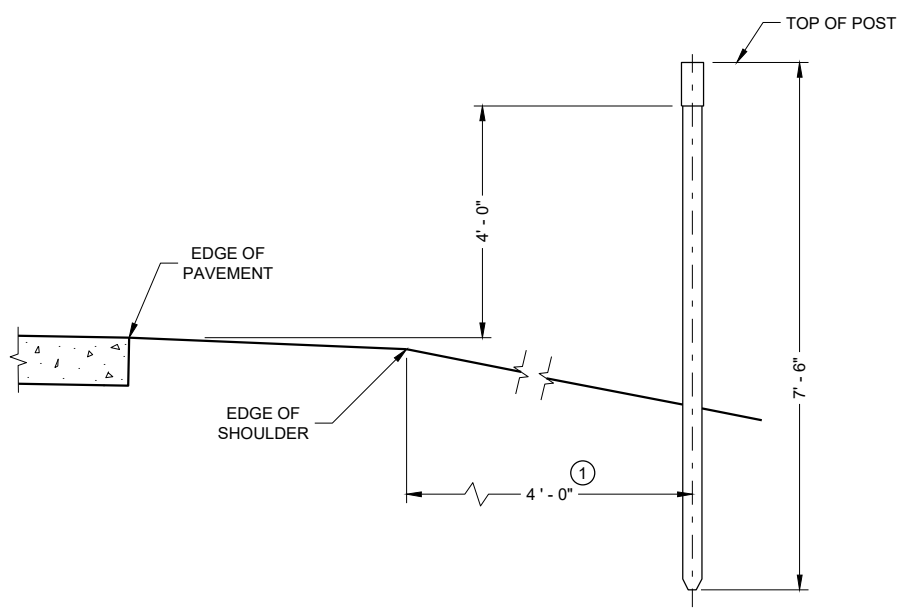
REFLECTOR SPACING	LOCATION
* 100' C-C	RAMPS
400' C-C	MAINLINE

* START AT BEGINNING OF RAMP TAPER AND END AT END OF RAMP TAPER

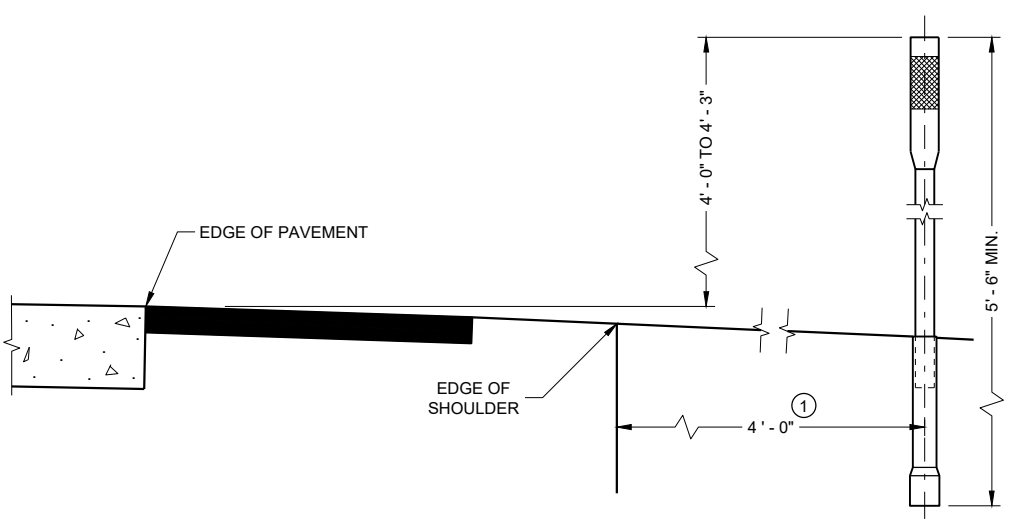
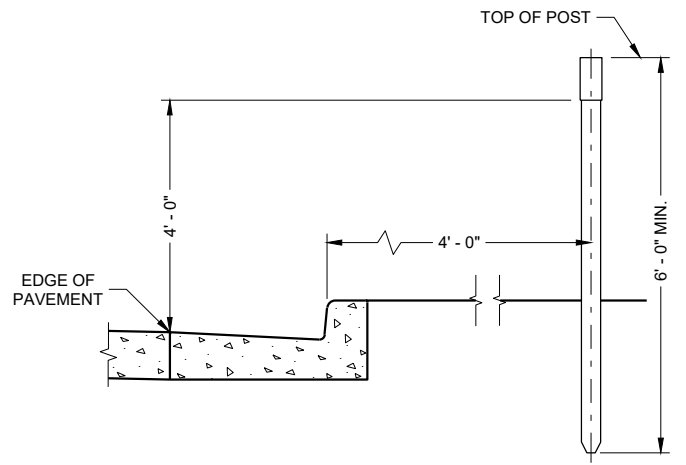
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

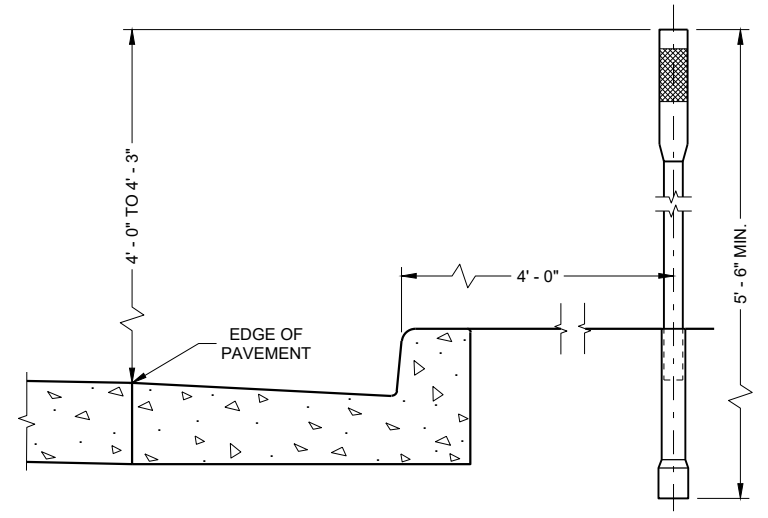
- ① DELINEATORS SHALL BE PLACED AT A CONSTANT DISTANCE FROM THE EDGE OF THE SHOULDER FOR THE LENGTH OF THE INSTALLATION.
- ② FURNISH TYPE H SHEETING FROM THE APPROVED PRODUCTS LIST.



TYPICAL INSTALLATIONS OF DELINEATOR POSTS



TYPICAL INSTALLATIONS OF FLEXIBLE DELINEATOR POSTS



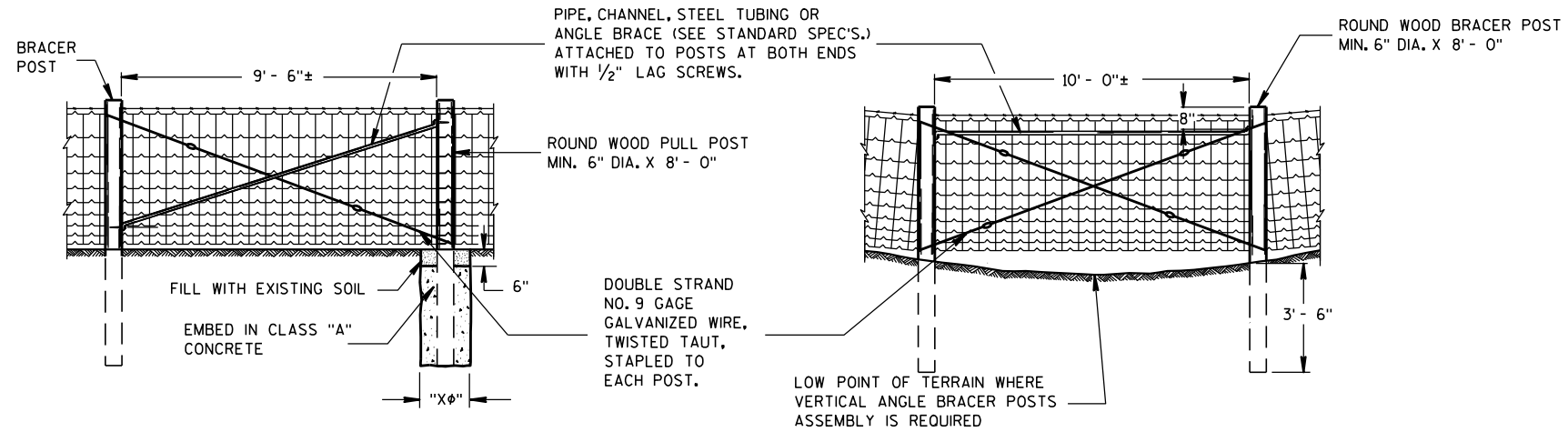
DELINEATOR POST WITH REFLECTIVE SHEETING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

NOTE: PULL OR STRETCHER POST ASSEMBLIES SHALL BE PLACED MIDWAY BETWEEN END POSTS AND CORNER POSTS WHERE A RUN OF FENCE EXCEEDS 660' BUT IS LESS THAN 1,320'. FOR RUNS OF FENCE IN EXCESS OF 1,320' MAXIMUM SPACING OF PULL OR STRETCHER POST ASSEMBLIES SHALL BE 660'± C-C.

ILLUSTRATION SHOWS POSITION OF STANDARD STEEL BRACE, DOUBLE STRAND GALVANIZED WIRE, AND THE POST TO BE EMBEDDED IN CONCRETE WHEN WIRE FENCE IS INSTALLED FROM LEFT TO RIGHT. THE BRACES SHALL BE POSITIONED ON THE OPPOSITE DIAGONALS AND THE OPPOSITE POST SHALL BE EMBEDDED IN CONCRETE WHEN WIRE FENCE IS INSTALLED FROM RIGHT TO LEFT.



PULL OR STRETCHER POSTS ASSEMBLY

VERTICAL ANGLE BRACER POSTS ASSEMBLY

GENERAL NOTES

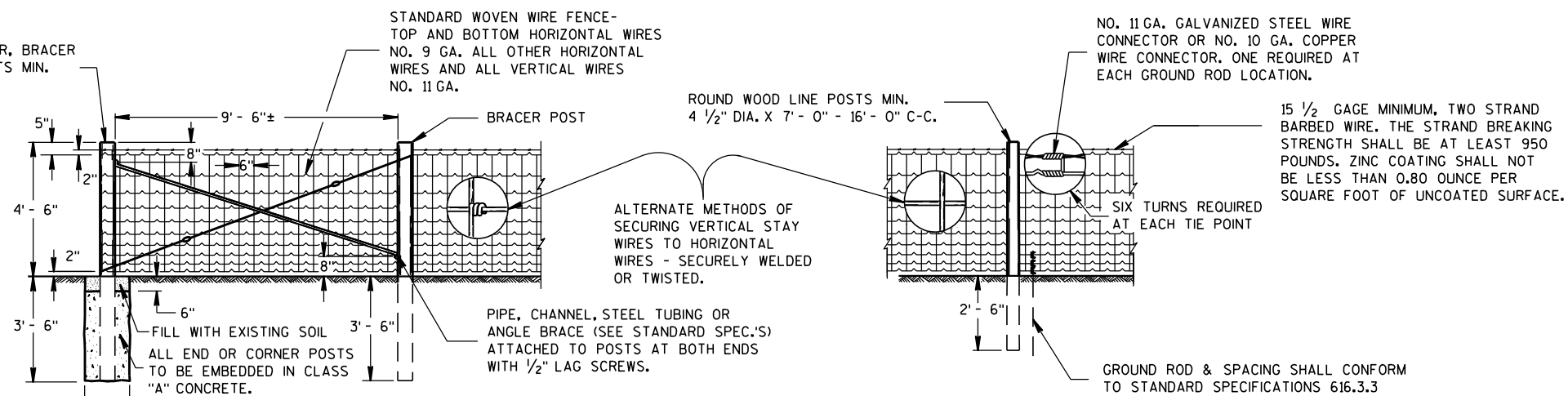
"Xφ" = DIAMETER OF THE POST PLUS 12".

FENCE STAPLES SHOULD NEVER BE DRIVEN VERTICALLY INTO WOOD POSTS (WITH BOTH LEGS PARALLEL WITH THE WOOD GRAIN). DOING SO CAN SEPARATE THE GRAIN AND SIGNIFICANTLY REDUCE THE HOLDING POWER. ROTATING THE STAPLES SLIGHTLY OFF VERTICAL STRADDLES THE GRAIN AND PROVIDES MORE RESISTANCE TO PULL-OUT.

DO NOT STAPLE WIRE TIGHT TO THE LINE POSTS. ALLOW MOVEMENT OF WIRE FOR EXPANSION AND CONTRACTION. STAPLE ARRANGEMENT SHALL BE THE SAME FOR ALL OTHER POSTS EXCEPT THAT THEY SHALL BE DRIVEN TIGHT TO POSTS. ALL STAPLES SHALL BE 2" X 9 GAGE AND SHALL BE MANUFACTURED FROM GALVANIZED WIRE OR HOT DIP GALVANIZED AFTER FORMING. STAPLES SHALL HAVE SLASH-CUT POINTS.

FENCE SHALL BE LOCATED 3'-0" INSIDE THE RIGHT OF WAY LINE UNLESS OTHERWISE INDICATED ON THE PLANS.

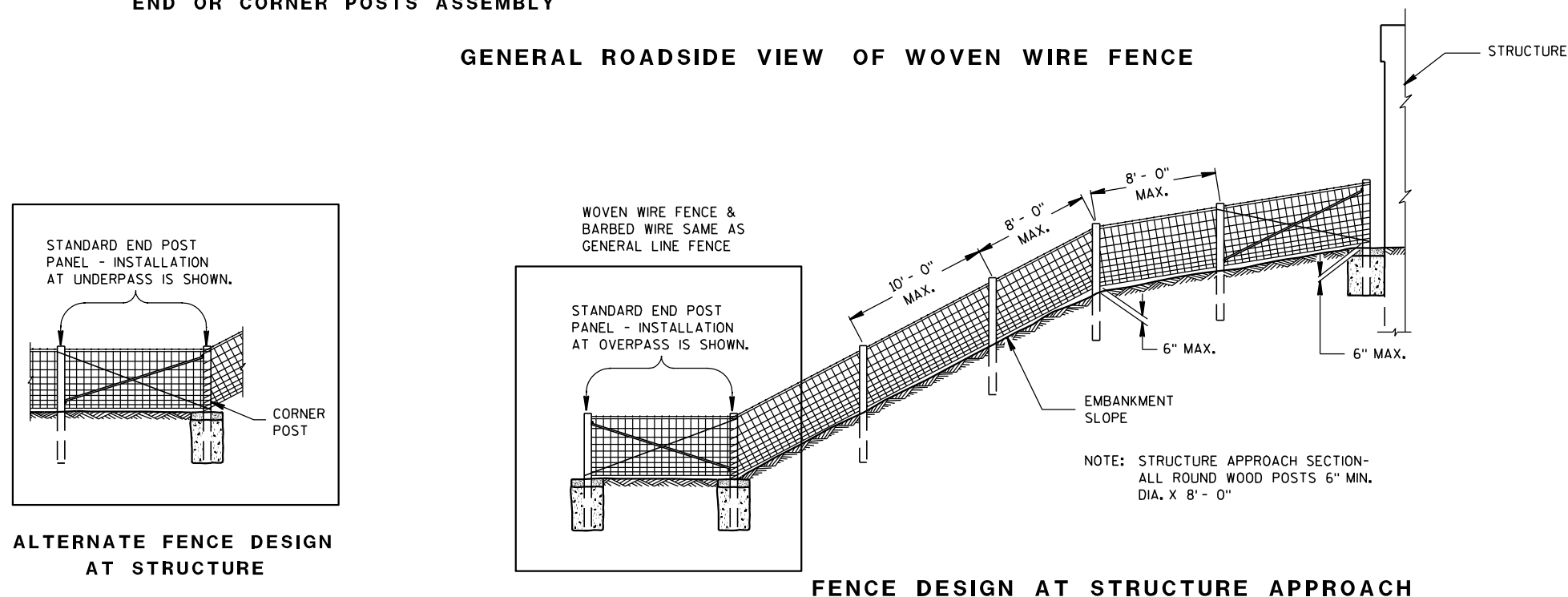
ROUND WOOD END, CORNER, BRACER OR VERTICAL ANGLE POSTS MIN. 6" DIA. X 8' - 0"



END OR CORNER POSTS ASSEMBLY

LINE FENCE CONSTRUCTION

GENERAL ROADSIDE VIEW OF WOVEN WIRE FENCE



ALTERNATE FENCE DESIGN AT STRUCTURE

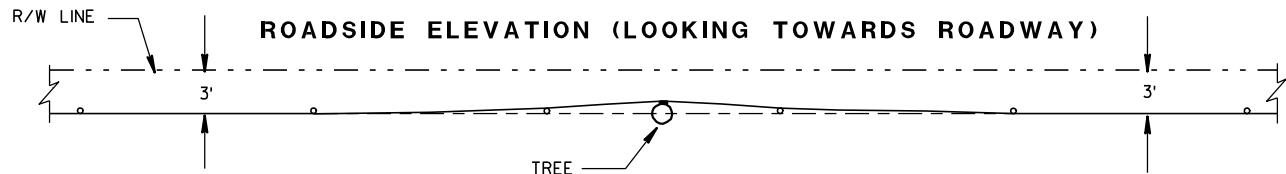
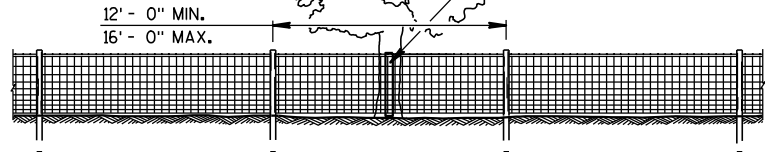
FENCE DESIGN AT STRUCTURE APPROACH

FENCE WOVEN WIRE

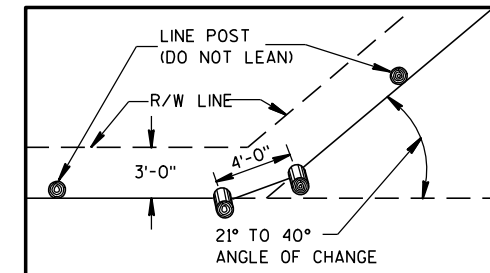
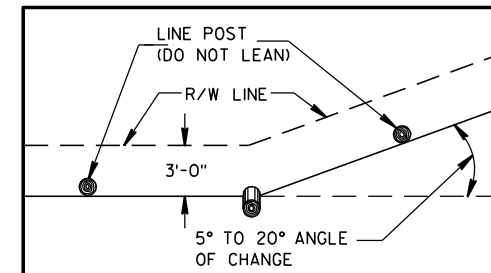
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

NOTE: TREE IN NORMAL FENCE LINE SPECIFICALLY ORDERED BY ENGINEER TO REMAIN IN PLACE.

2" X 6" DOUGLAS FIR OR SO. YELLOW PINE PLACED BETWEEN TREE AND WOVEN WIRE FENCE. WOVEN WIRE FENCE AND BARBED WIRE TO BE STAPLED TO 2" X 6" LIKE AS TO LINE POST. 2" X 6" NOT FASTENED TO TREE.



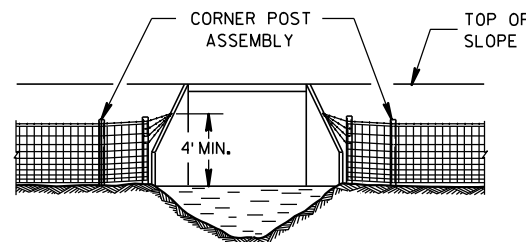
**PLAN VIEW
FENCE DESIGN AT TREES REMAINING
IN NORMAL FENCE LINE**



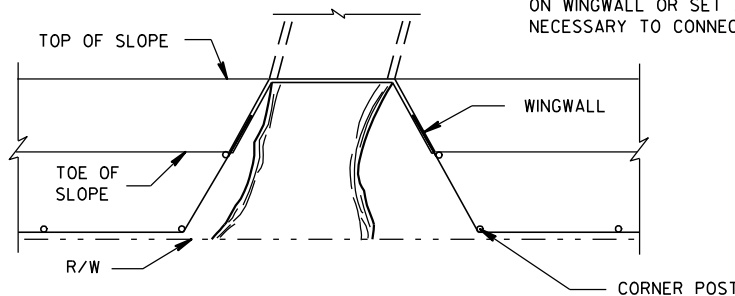
**PLAN VIEW
SINGLE POST CORNER
PLAN VIEW
DOUBLE POST CORNER
RIGHT OF WAY LINE CHANGE 40° AND LESS**

NOTE: SINGLE AND DOUBLE POSTS SHALL BE A MIN. 6" DIA. X 8'-0" WITH A LEAN OF 4" TOWARD THE OUTSIDE OF THE CURVE.

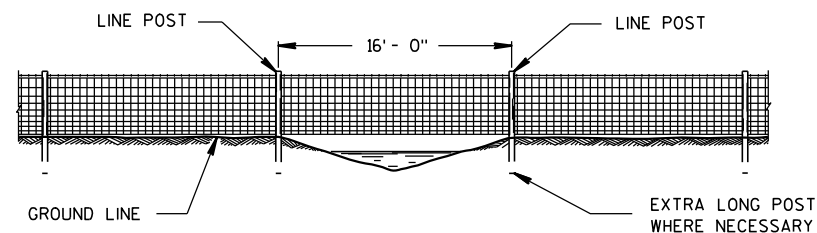
WHEN THE RIGHT OF WAY LINE CHANGE IS MORE THAN 40° USE THE CORNER OR STRETCHER POSTS ASSEMBLY.



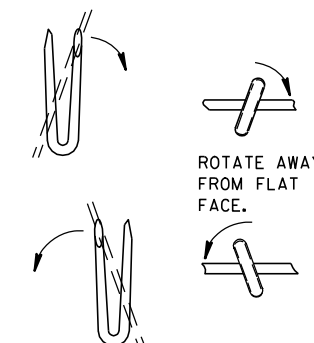
NOTE: PLACE A MINIMUM OF 4 STRANDS OF BARBED WIRE, 6" MAXIMUM CENTERS IN FAN SHAPE CONNECTED TO AN EYE BOLT ON WINGWALL OR SET A LONE POST WHEN NECESSARY TO CONNECT BARBED WIRE.



FENCE INSTALLATION TO WINGWALLS



**FENCE CONSTRUCTION OVER STREAM
COURSES OF 15 FT. OR LESS IN WIDTH**



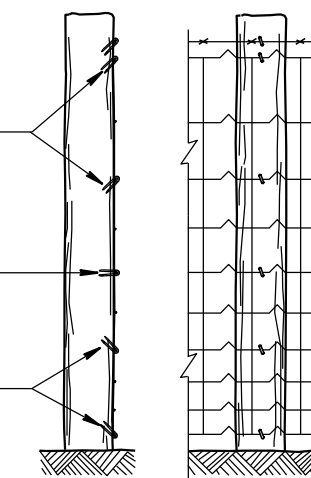
LINE POST

NOTE: WHEN POSTS ARE DRIVEN THE SMALL END SHALL BE DOWN.

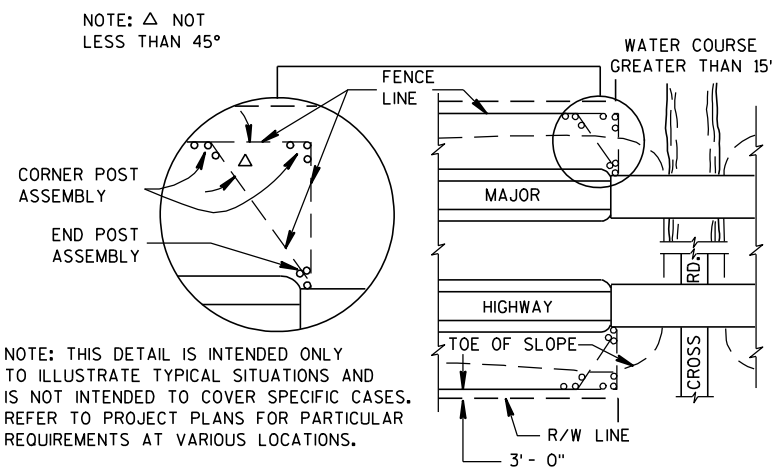
STAPLES SLOPED DOWNWARD FOR SUSTAINED GRADES AND OVER KNOLLS.

STAPLES LEVEL FOR LEVEL GROUND.

SLOPE UPWARDS WHEN FENCE TENDS TO LIFT.

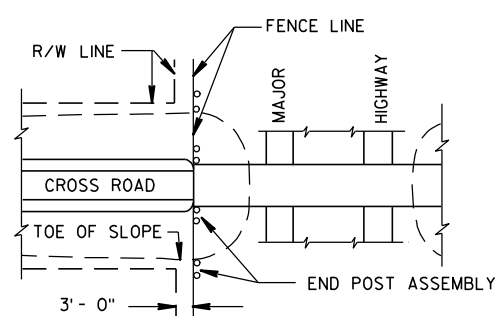


**END ELEVATION
FARM SIDE ELEVATION
FENCE MOUNTING DETAIL**



**PLAN VIEW
MAJOR HIGHWAY OVERPASS OR STREAM COURSE
CROSSING OF GREATER THAN 15 FT. IN WIDTH**

FENCE LOCATION AT STRUCTURES

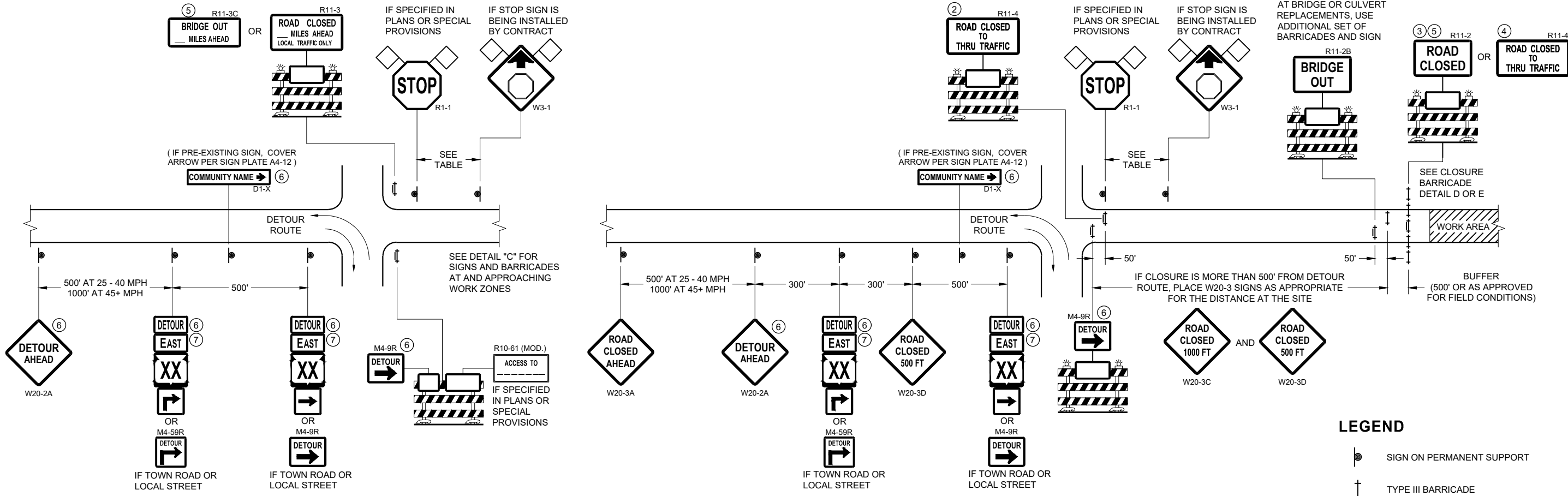


**PLAN VIEW
MAJOR HIGHWAY UNDERPASS**

FENCE WOVEN WIRE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/4/2008 /S/ Jerry H. Zogg
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA

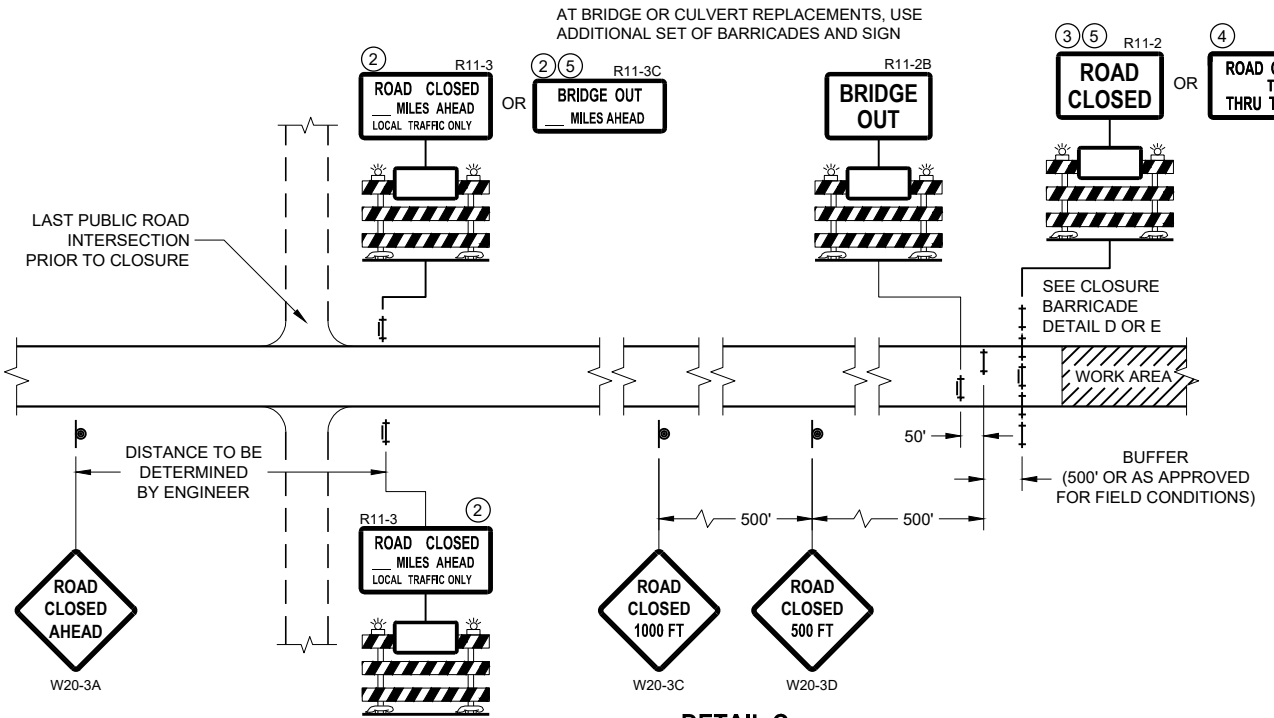
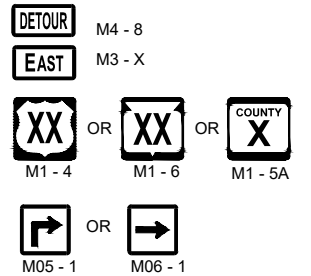


DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE LESS THAN 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TYPE "A" WARNING LIGHT (FLASHING)
 - WORK AREA
 - FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

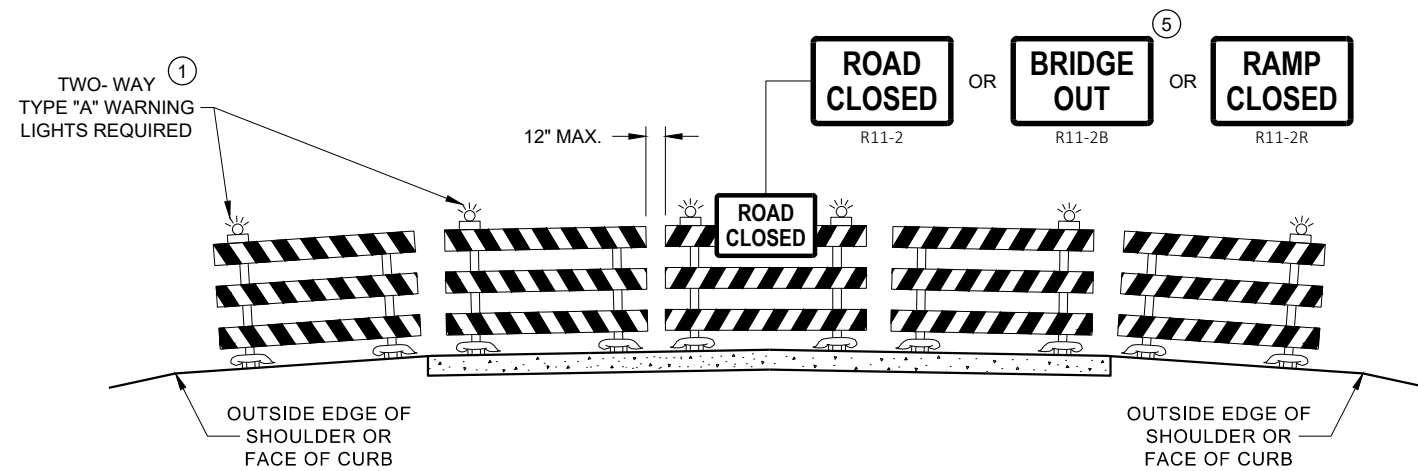
SEE SDD 15C2-SHEET "b"
 FOR GENERAL NOTES
 AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
 FOR MAINLINE CLOSURES**

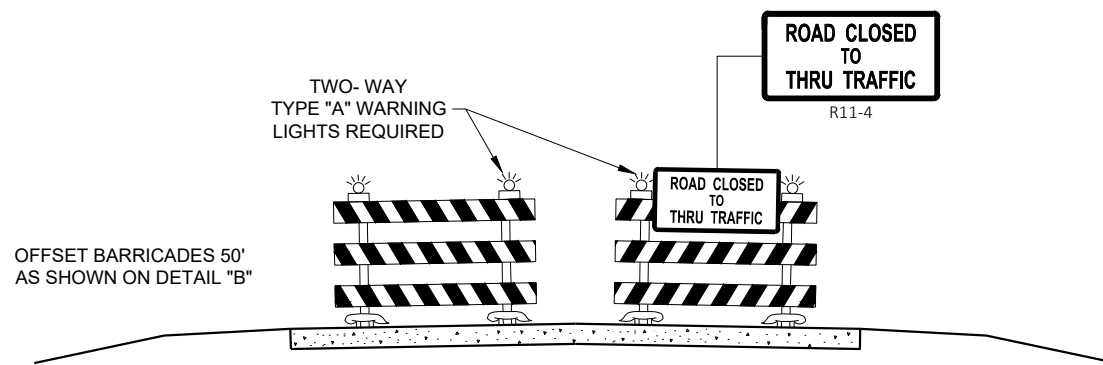
STATE OF WISCONSIN
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APPROVED
 November 2018 /S/ Andrew Heidtke
 DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

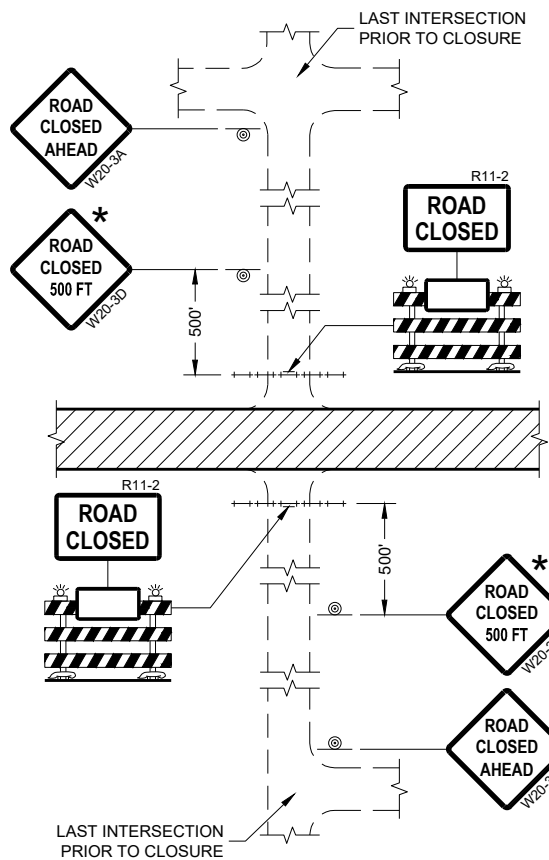
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

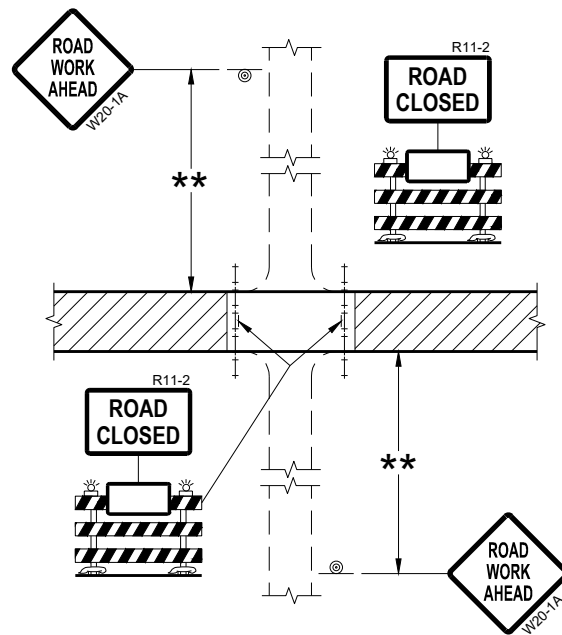
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DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

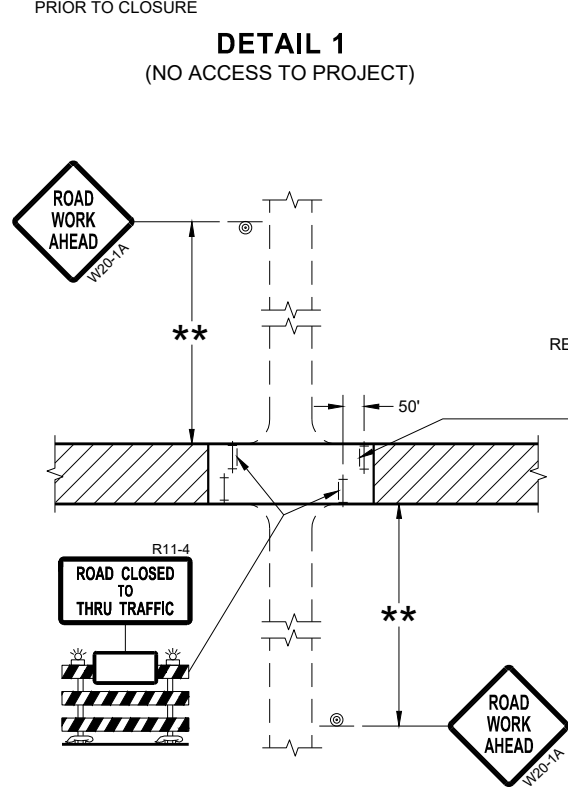
FHWA



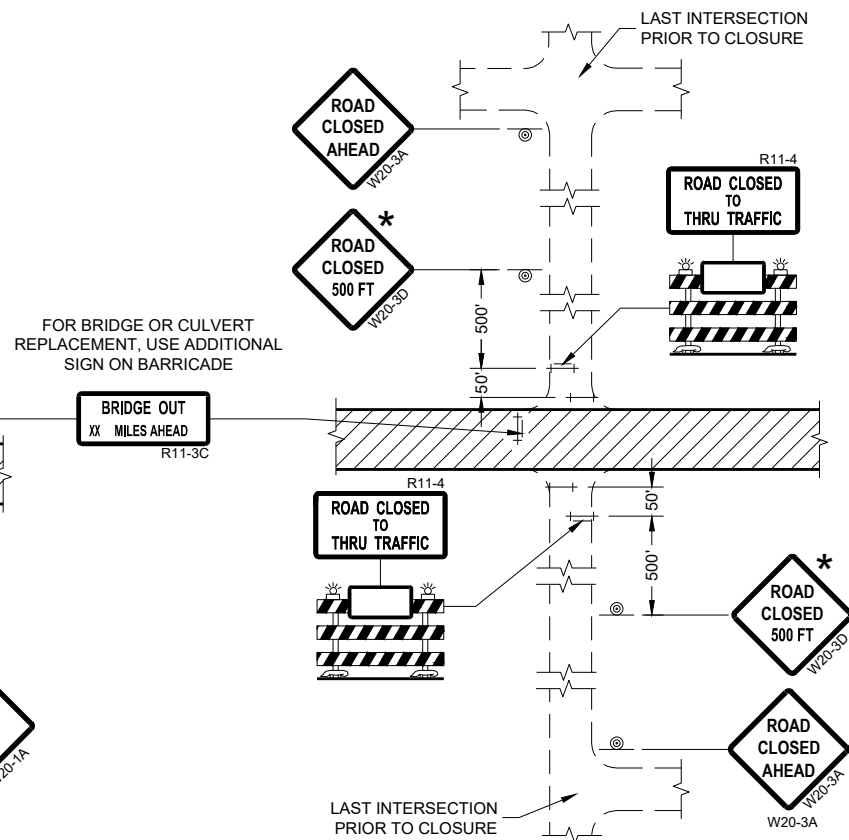
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

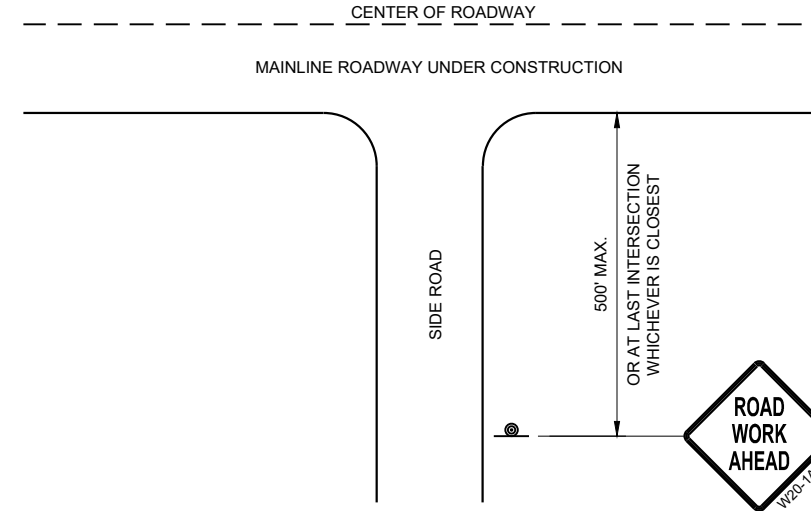
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

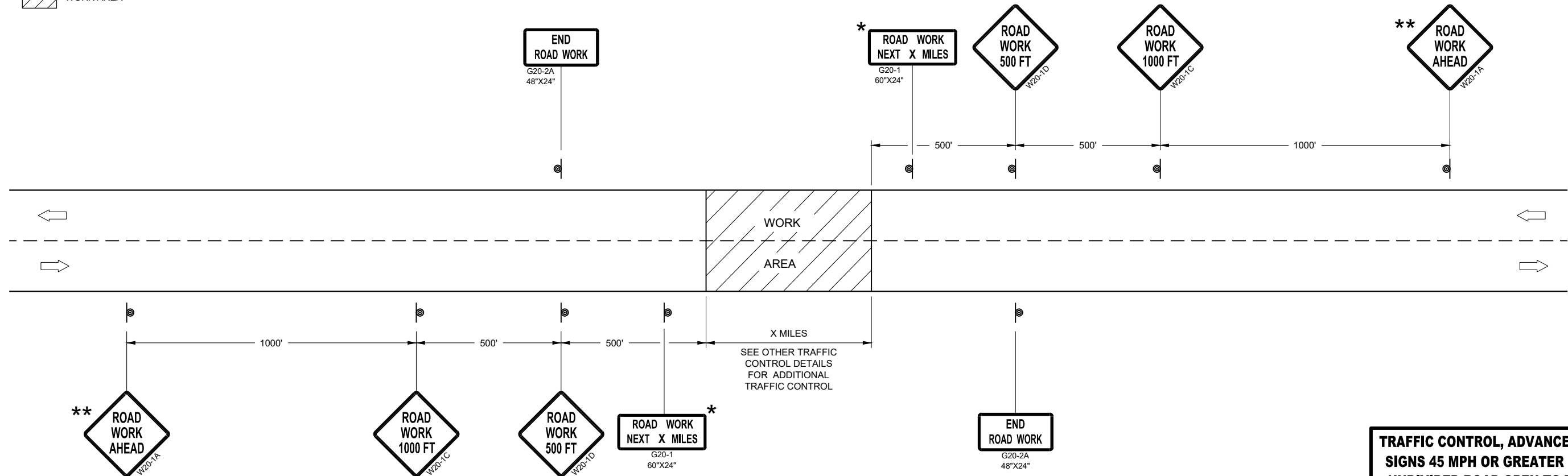
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



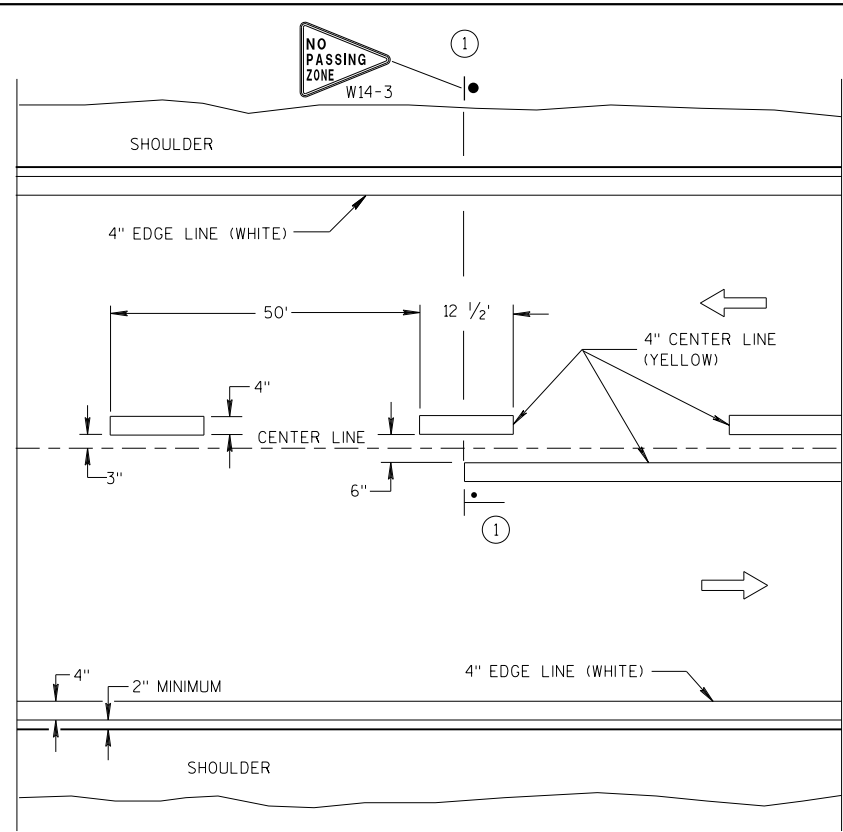
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFICE**

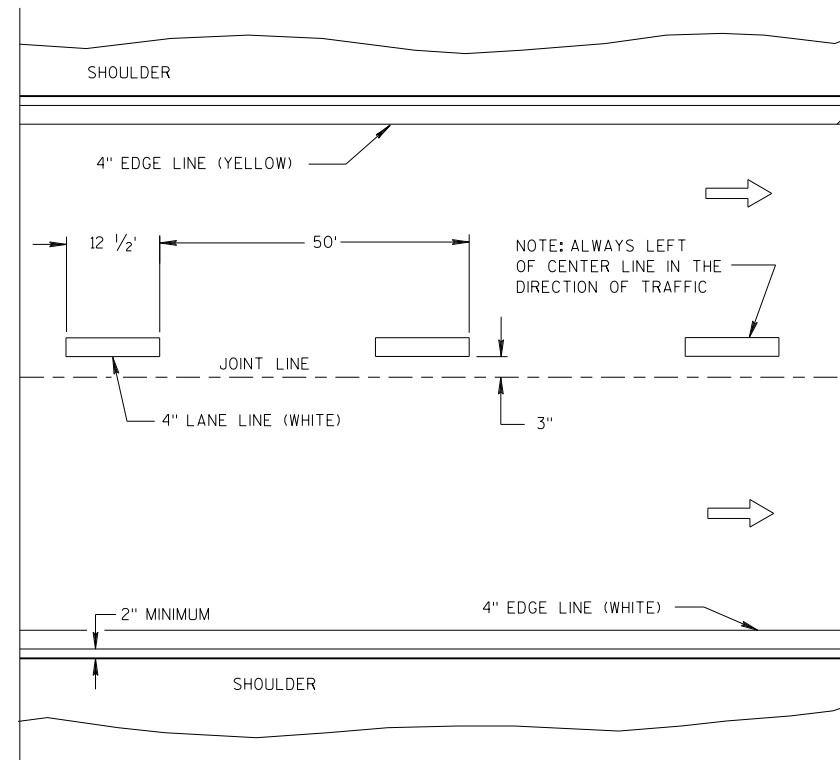
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

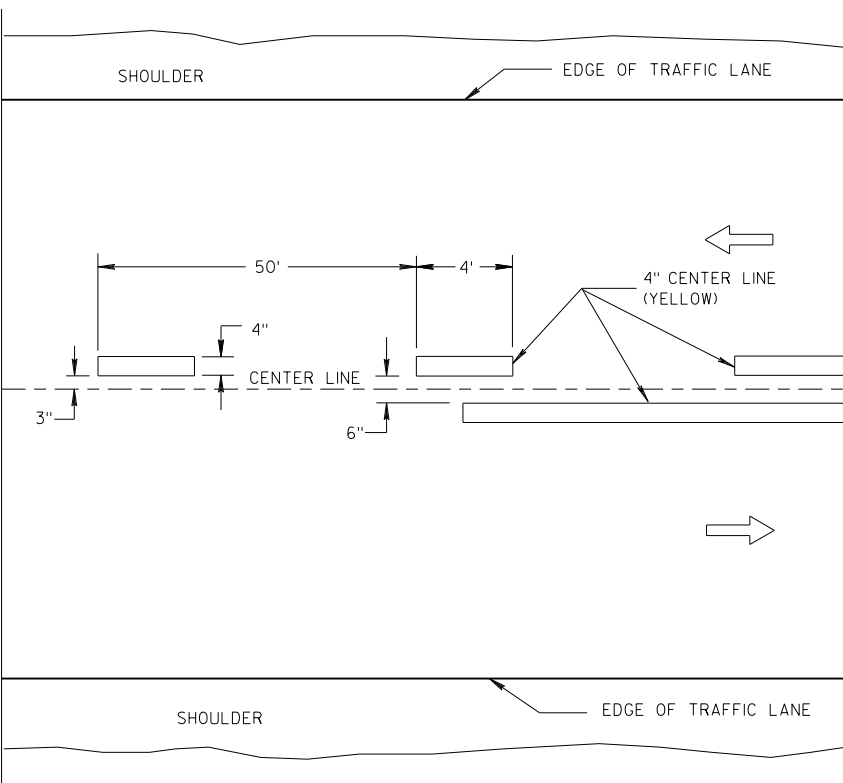


TWO WAY TRAFFIC

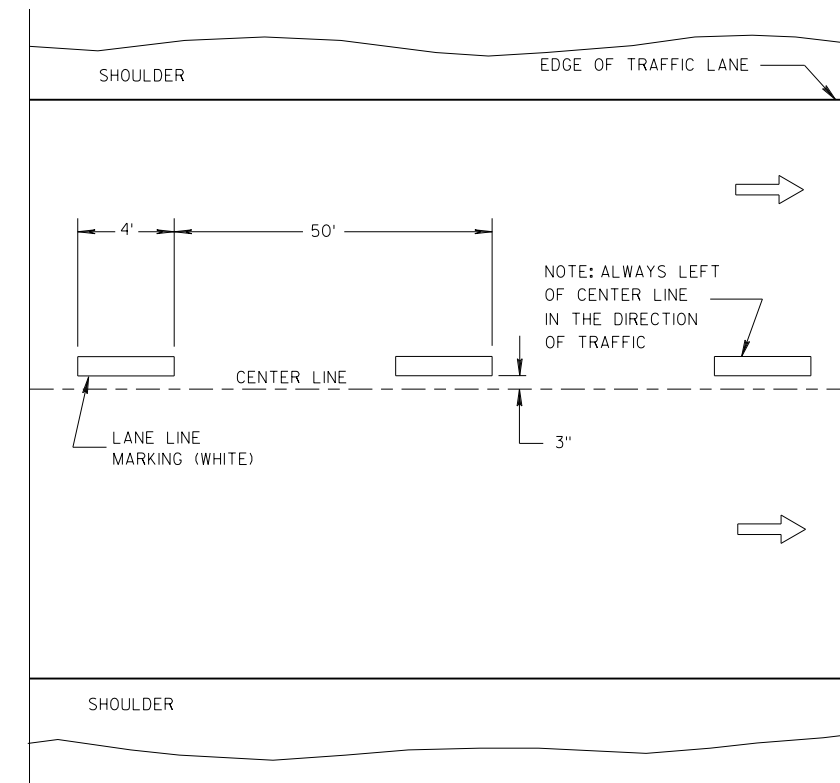


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

LEGEND

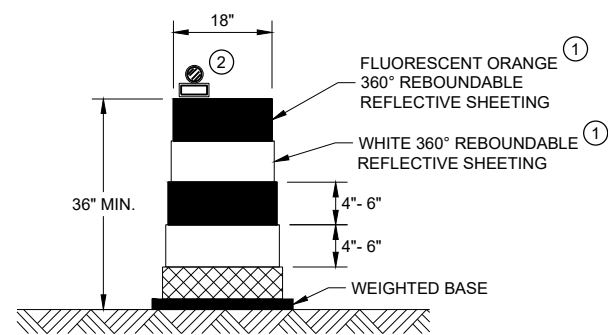
—●— "T" MARKING

● POST MOUNTED SIGN

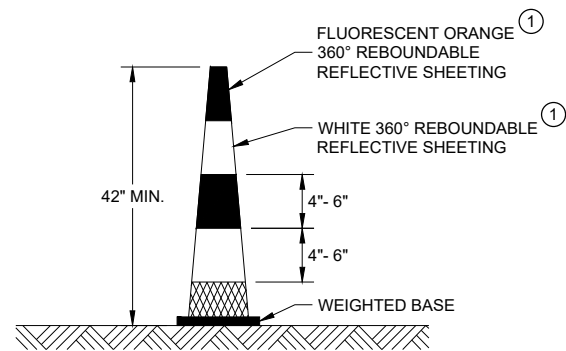
LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

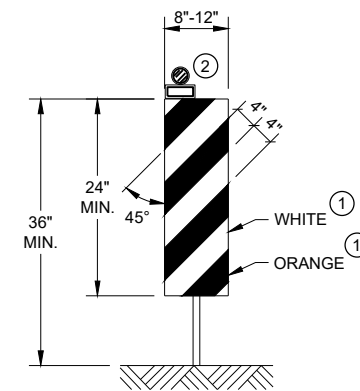


DRUM



42" CONE

DO NOT USE IN TAPERS
 1/2 SPACING OF DRUMS

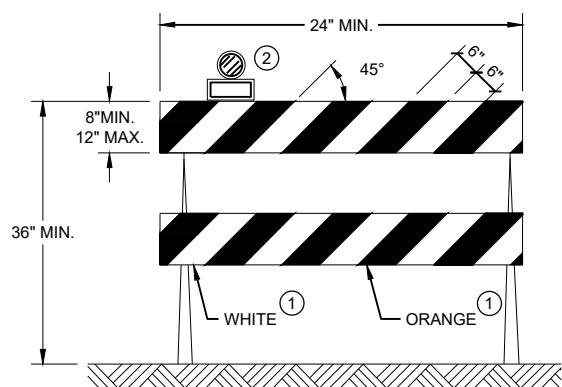


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

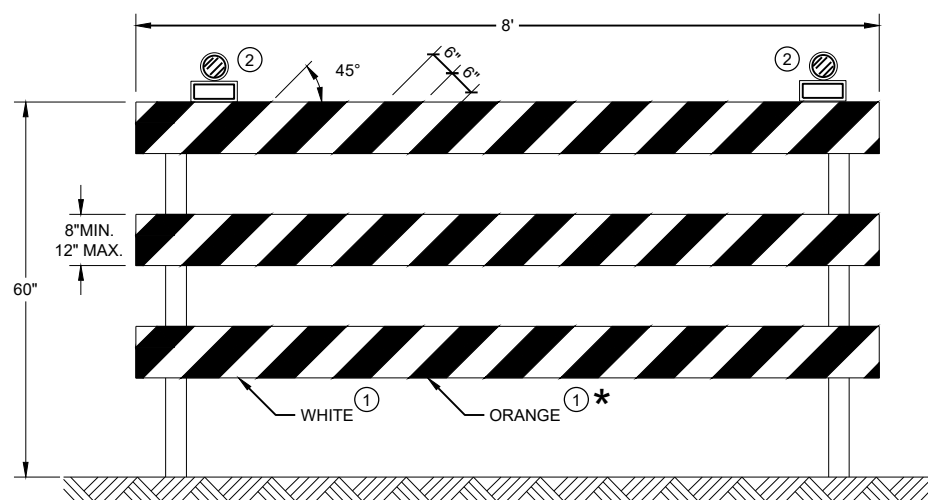
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.








TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

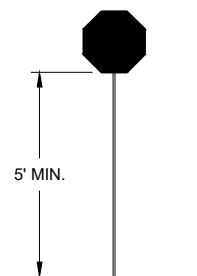
WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
 - ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



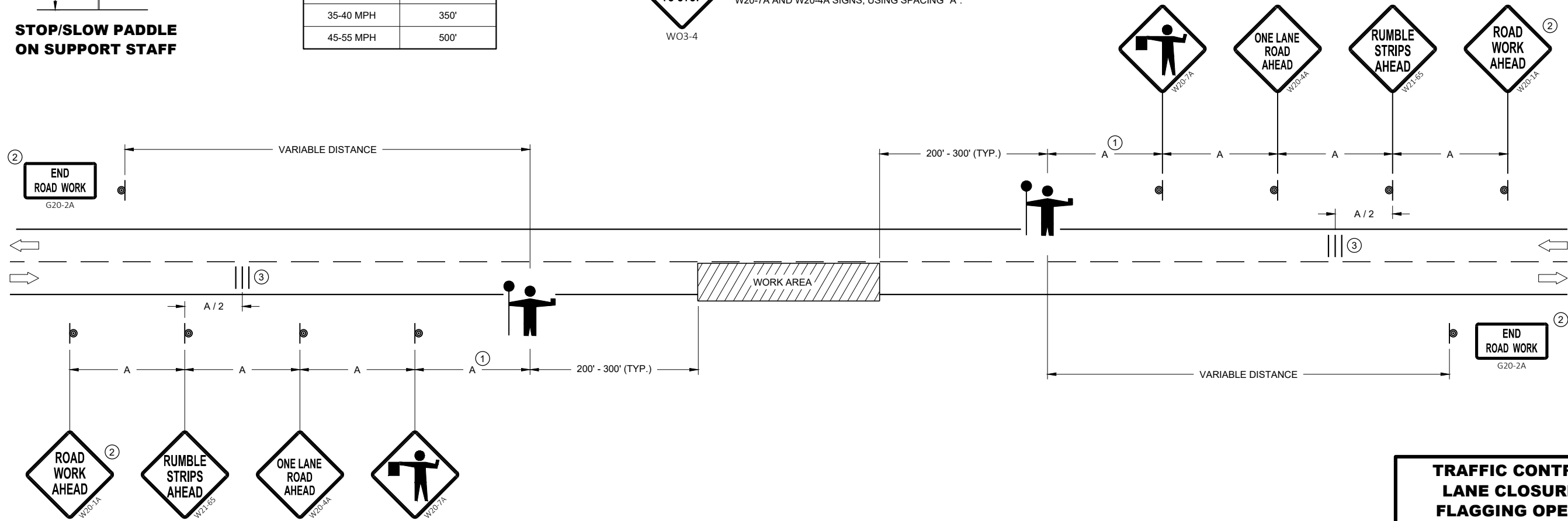
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION


APPROVED
DATE May 2019 /S/ Andrew Heidtke
WORK ZONE ENGINEER

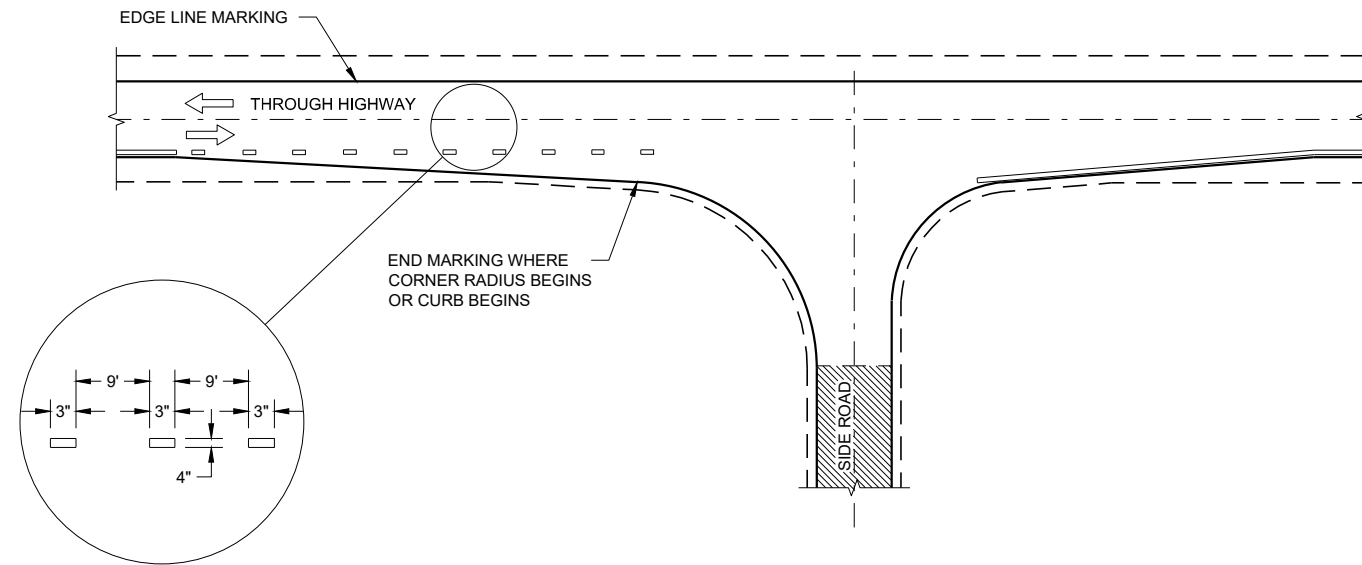
FHWA

GENERAL NOTES

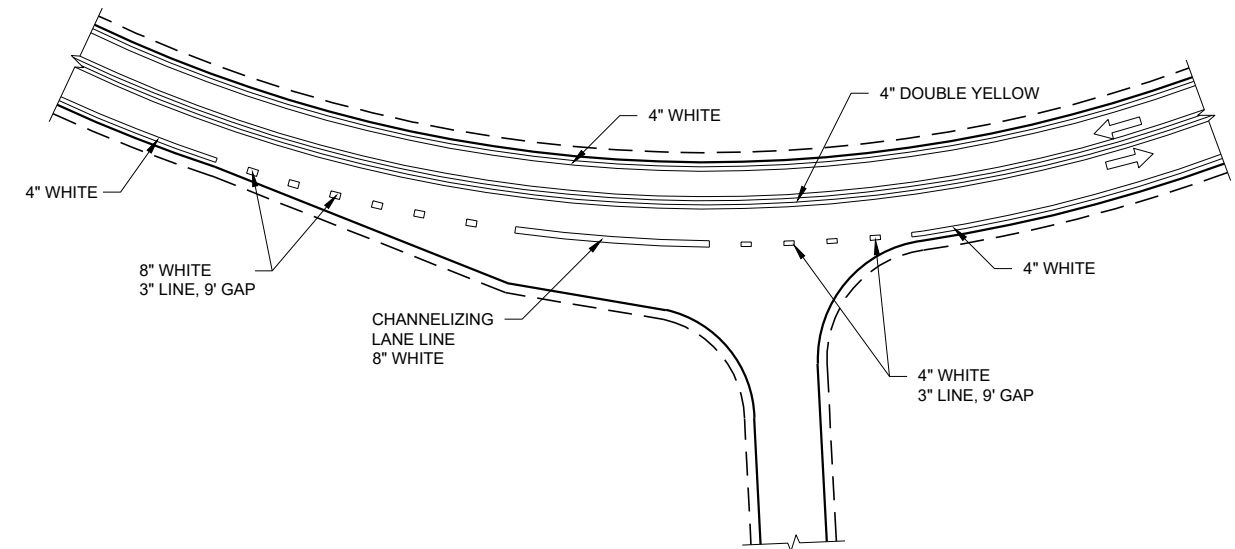
OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER

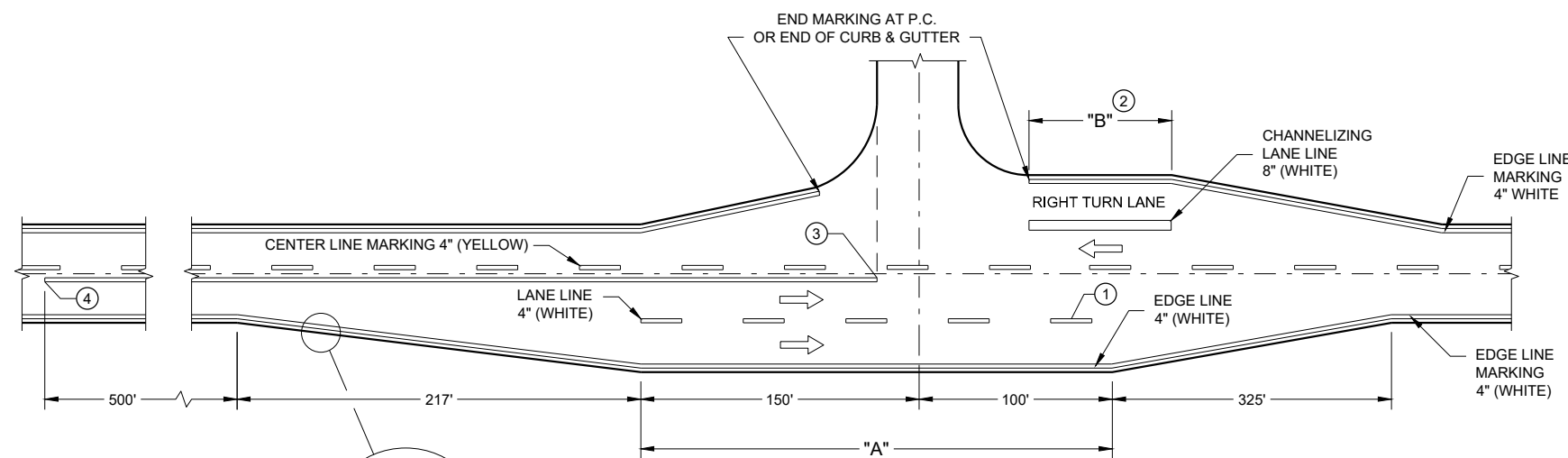
ARROW SYMBOL () SHOWS DIRECTION OF TRAVEL



MINOR INTERSECTION

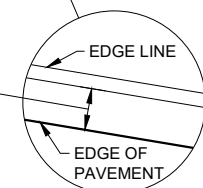


INTERSECTION ON OUTSIDE OF CURVE



MAJOR INTERSECTIONS (INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES



PAVEMENT MARKING (INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND





-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

TABLE A

SHOULDER TAPER LENGTH (FEET)					BUFFER SPACE (FEET)
S \ W	4	6	8	10	
30	20	30	40	50	200
35	30	45	55	70	250
40	40	55	75	90	305
45	60	90	120	150	360
50	70	100	135	170	425
55	75	110	150	185	495

W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH

L = WS AT 45 MPH OR GREATER
L = WS² / 60 AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = 1/3L

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

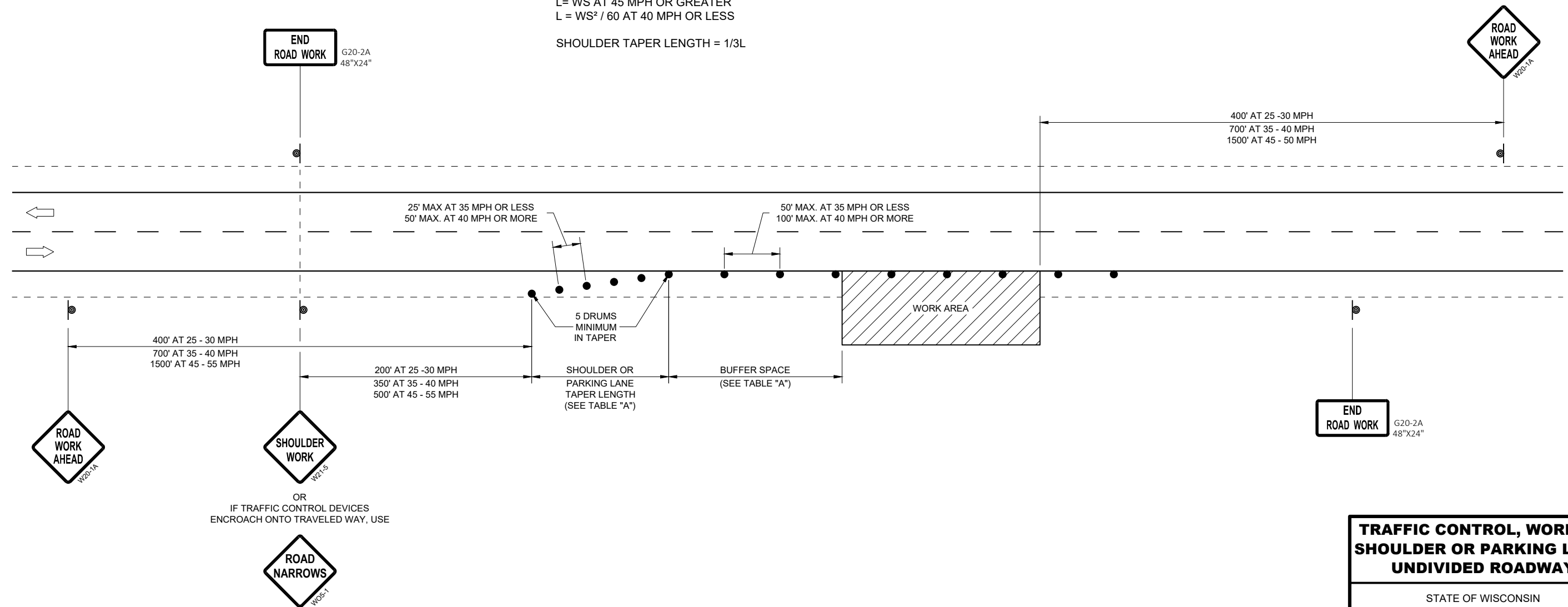
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

6



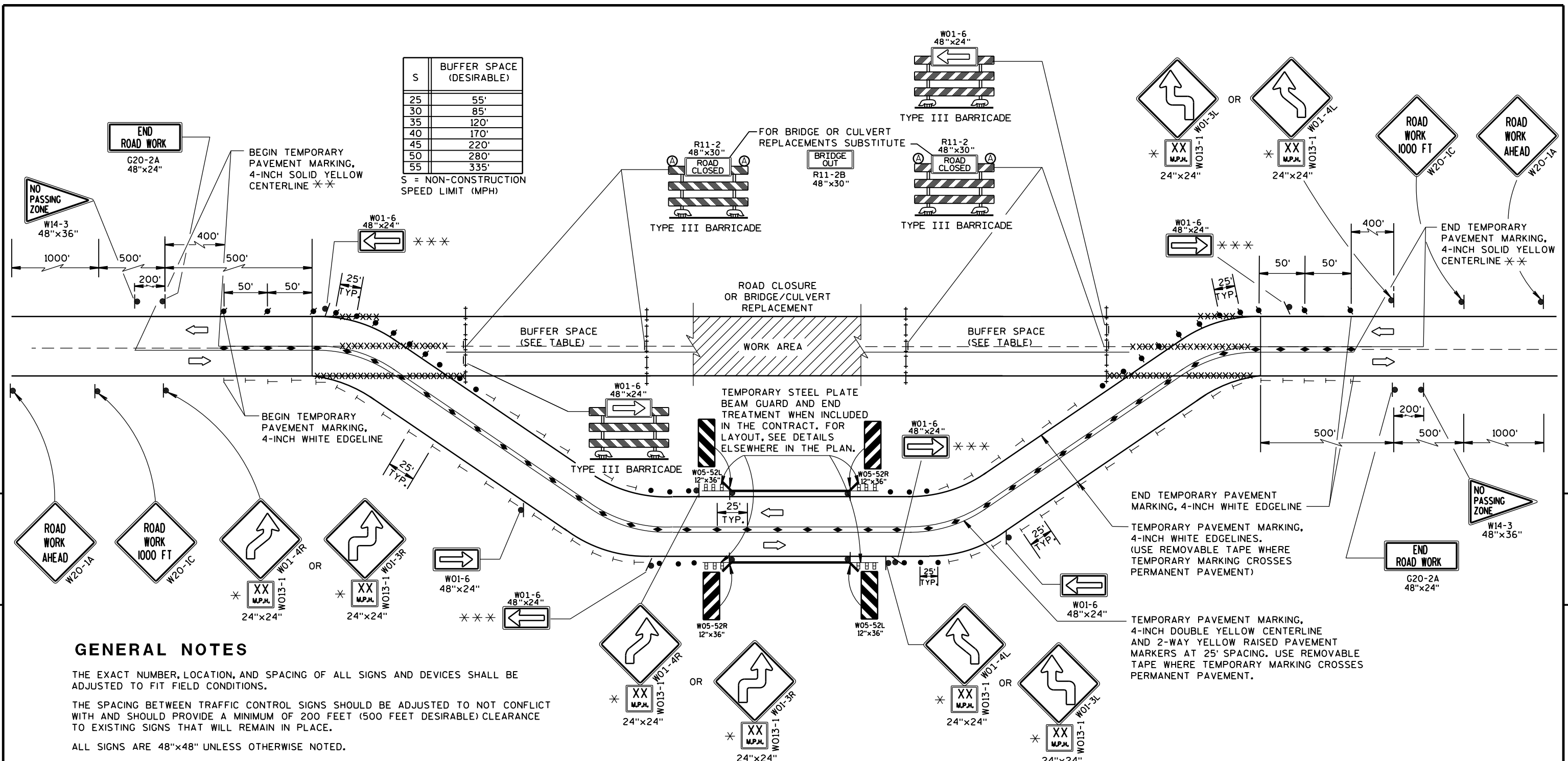
SDD 15D28 - 03

SDD 15D28 - 03

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED August 2019 DATE	/S/ Andrew Heidtke STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

S	BUFFER SPACE (DESIRABLE)
25	55'
30	85'
35	120'
40	170'
45	220'
50	280'
55	335'

S = NON-CONSTRUCTION SPEED LIMIT (MPH)



GENERAL NOTES

- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.
- "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED.
- EQUIPMENT, VEHICLES, OR MATERIAL SHOULD NOT BE STORED IN BUFFER SPACE.
- * IF ADVISORY SPEED IS GREATER THAN 30 MPH, USE THE W01-4 SIGN. IF ADVISORY SPEED IS 30 MPH OR LESS, USE THE W01-3 SIGN.
- ** WHEN THE DISTANCE TO/FROM THE NEXT CLOSEST NO-PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
- *** OMIT THESE W01-6 SIGNS IF THE ADVISORY SPEED OF THE CURVE IS GREATER THAN 30 MPH.

LEGEND

- SIGN ON PERMANENT SUPPORT
- ⦿ TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY-BURN LIGHT
- TRAFFIC CONTROL DRUM
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- Ⓐ TYPE "A" WARNING LIGHT (FLASHING)
- TEMPORARY DELINEATOR, (WHITE) (SINGLE DELINEATOR)
- ◆ TEMPORARY RAISED PAVEMENT MARKERS (TWO-WAY YELLOW)
- XXX REMOVE PAVEMENT MARKING
- ➡ DIRECTION OF TRAFFIC
- ▬▬▬ TEMPORARY STEEL PLATE BEAM GUARD AND END TREATMENT
- ▨ WORK AREA

TRAFFIC CONTROL, TEMPORARY BYPASS ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: Sept. 2015 /S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

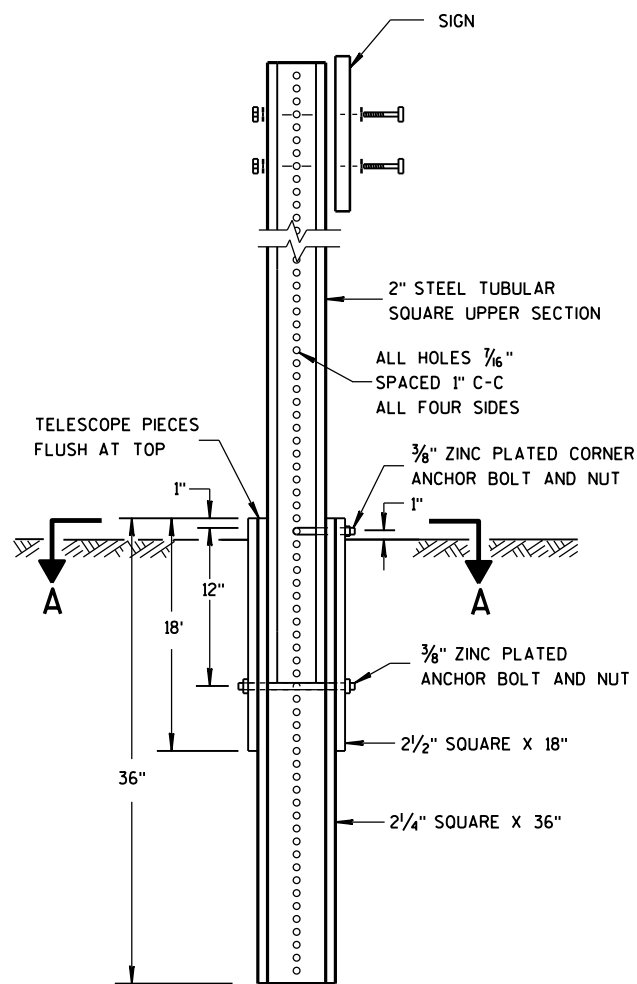
FHWA

6

6

S.D.D. 15 D 31-3

S.D.D. 15 D 31-3



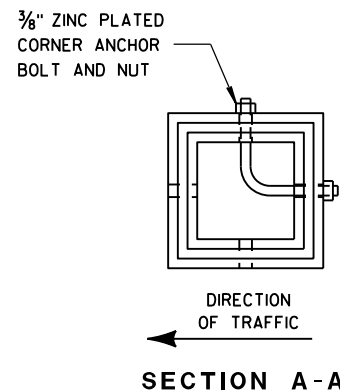
DETAIL OF TUBULAR STEEL SIGN POST

TUBULAR STEEL POSTS

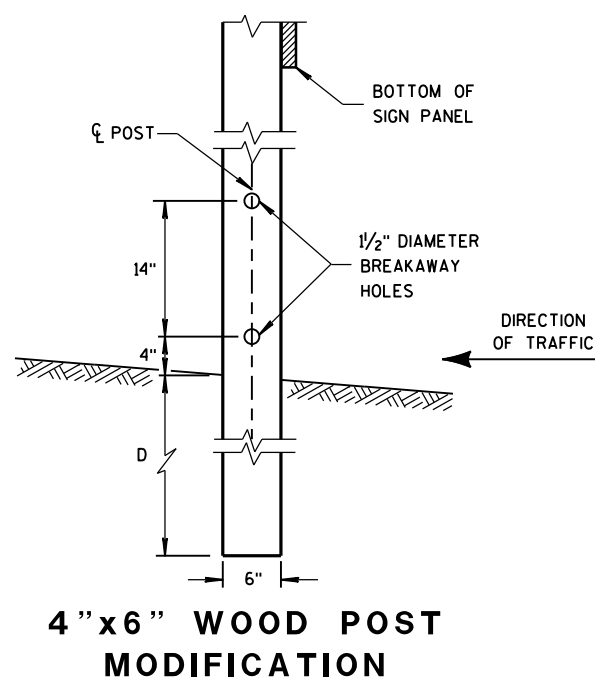
AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

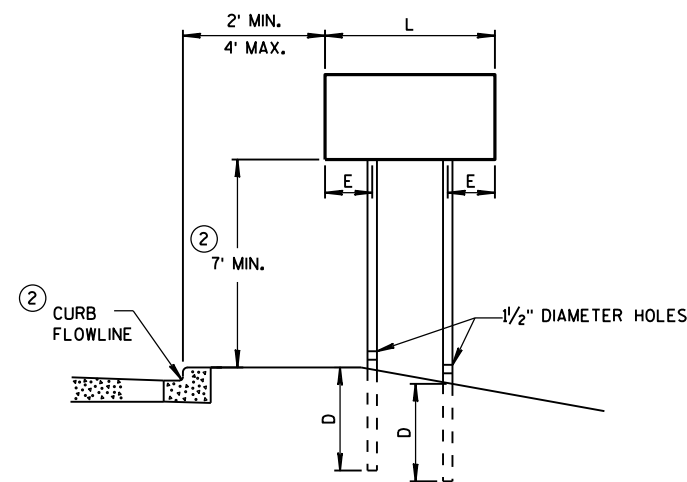
SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.



SECTION A-A



4" X 6" WOOD POST MODIFICATION

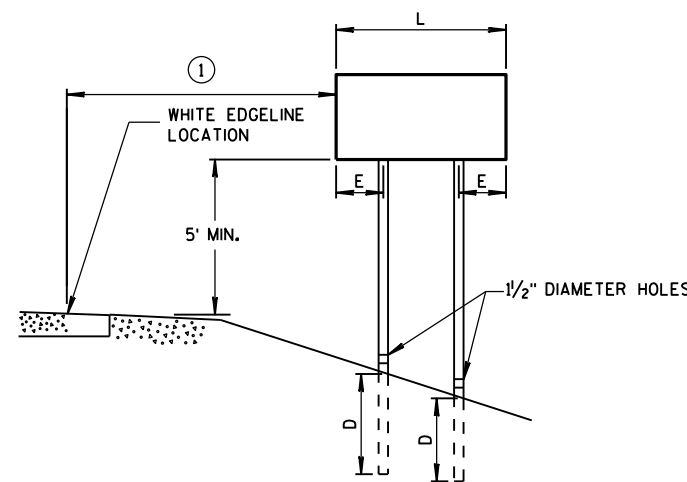


URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



RURAL AREA

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

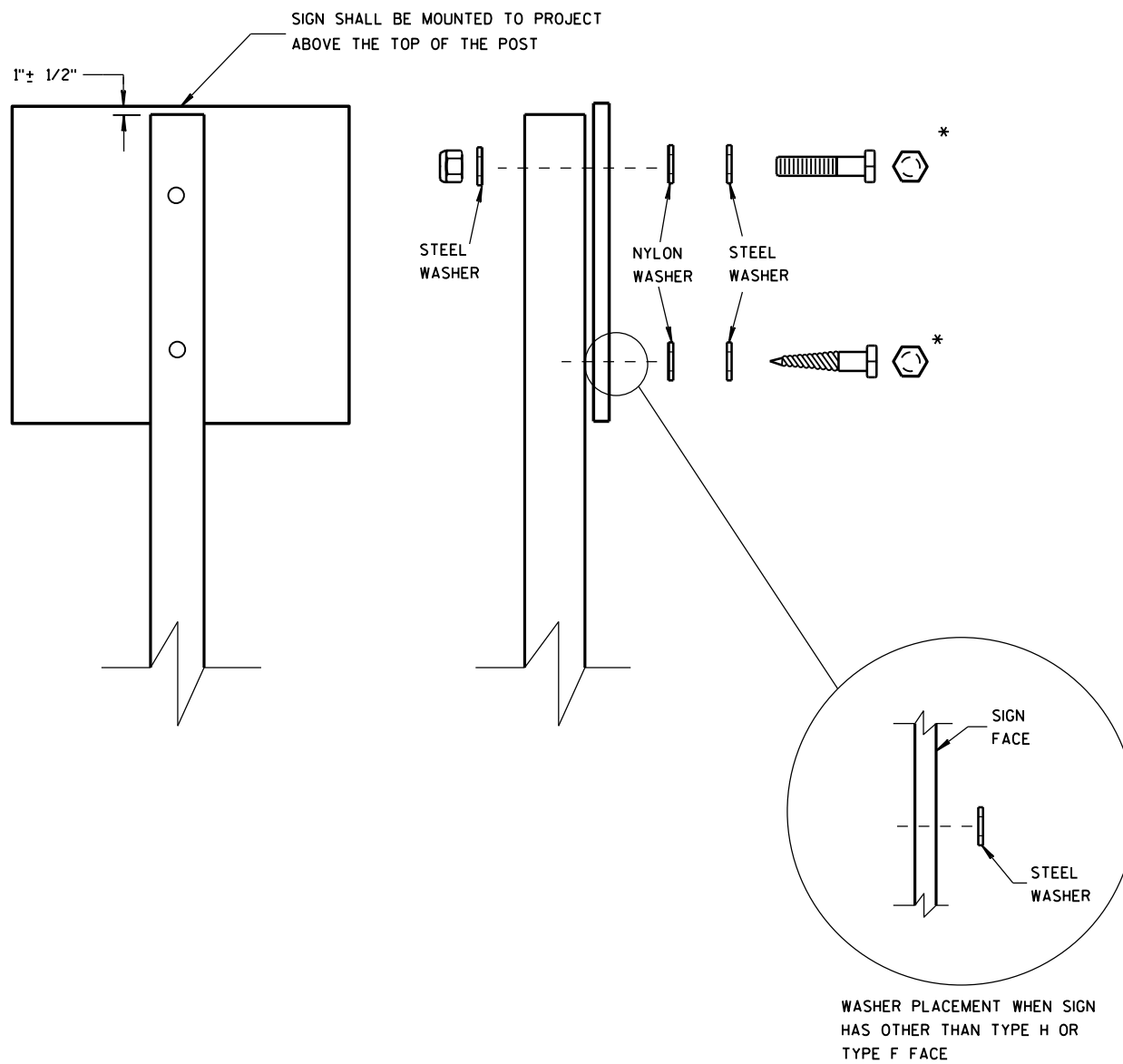
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3

B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" x 3"

MACHINE BOLTS - 5/16" x 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" x 3-1/4" LENGTH W/ NUTS

RIVETS - 5/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL

1-1/4" O.D. x 3/8" I.D. x .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/s/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
203.0200	REMOVING OLD STRUCTURE STA. 237+73	LS	1
206.2000	EXCAVATION FOR STRUCTURES CULVERTS C-18-49	LS	1
210.2500	BACKFILL STRUCTURE TYPE B	TON	1,670
311.0115	BREAKER RUN	CY	102
504.0100	CONCRETE MASONRY CULVERTS	CY	160
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	24,240
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	2,970
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	30
606.0300	RIPRAP HEAVY	CY	33
645.0105	GEOTEXTILE TYPE C	SY	335
645.0120	GEOTEXTILE TYPE HR	SY	96
NON-BID ITEMS			
	FILLER	SIZE	¾"

DESIGN DATA

LIVE LOAD:
 DESIGN LOADING: HL-93
 INVENTORY RATING FACTOR: RF = 1.05
 OPERATING RATING FACTOR: RF = 1.35
 WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 250 (KIPS)

EARTHLOAD:
 DESIGNED FOR 5 TO 7 FT. OF FILL.

MATERIAL PROPERTIES:
 CONCRETE MASONRY: $f'_c = 3,500$ P.S.I.
 BAR STEEL REINFORCEMENT: $f_y = 60,000$ P.S.I.

HYDRAULIC DATA

100 YEAR FREQUENCY
 $Q_{100} = 250$ C.F.S.
 $VEL_{100} = 4.30$ F.P.S.
 $HW_{100} = EL. 909.04$
 WATERWAY AREA = 96 SQ. FT.
 DRAINAGE AREA = 1.2 SQ. MI.
 ROADWAY OVERTOPPING = N/A
 SCOUR CRITICAL CODE = 8

2 YEAR FREQUENCY

$Q_2 = 60$ C.F.S.
 $VEL_2 = 2.5$ F.P.S.
 $HW_2 = EL. 906.35$

TRAFFIC VOLUME

USH 12
 ADT = 13,300 (2040)
 R.D.S. = 60 M.P.H.

- ▲ SEE CORNER DETAILS ON "APRON DETAILS" SHEET
- NAME PLATE LOCATION (SEE "APRON DETAILS" SHEET)
- * BUILD APRON AND END OF BOX LEVEL
- ⊙ INDICATES WING NUMBER
- ▲ VERT. CONST. JOINT (TYP.)

NOTE: STRUCTURE BACKFILL REQUIRED BEHIND ALL WING WALLS

STATE PROJECT NUMBER

7080-01-72

GENERAL NOTES

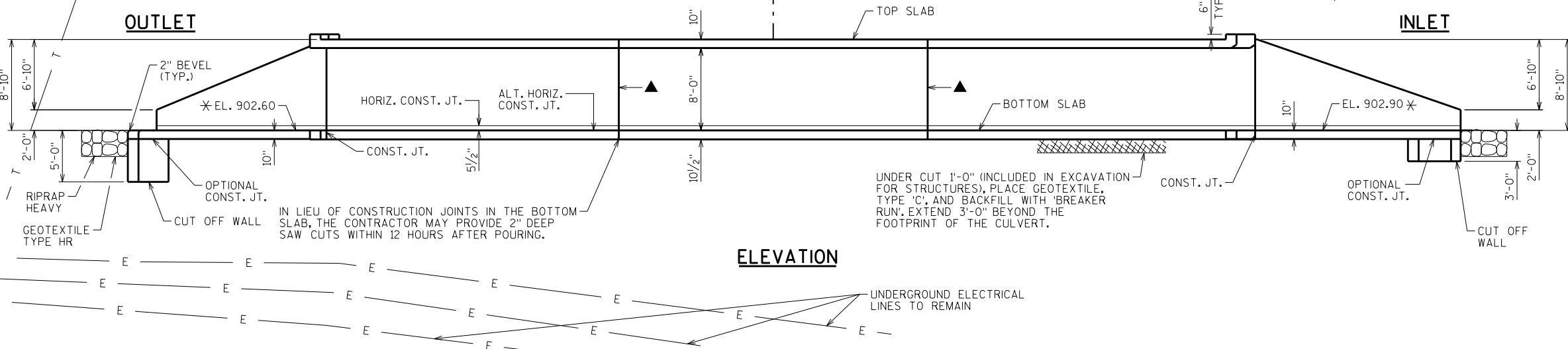
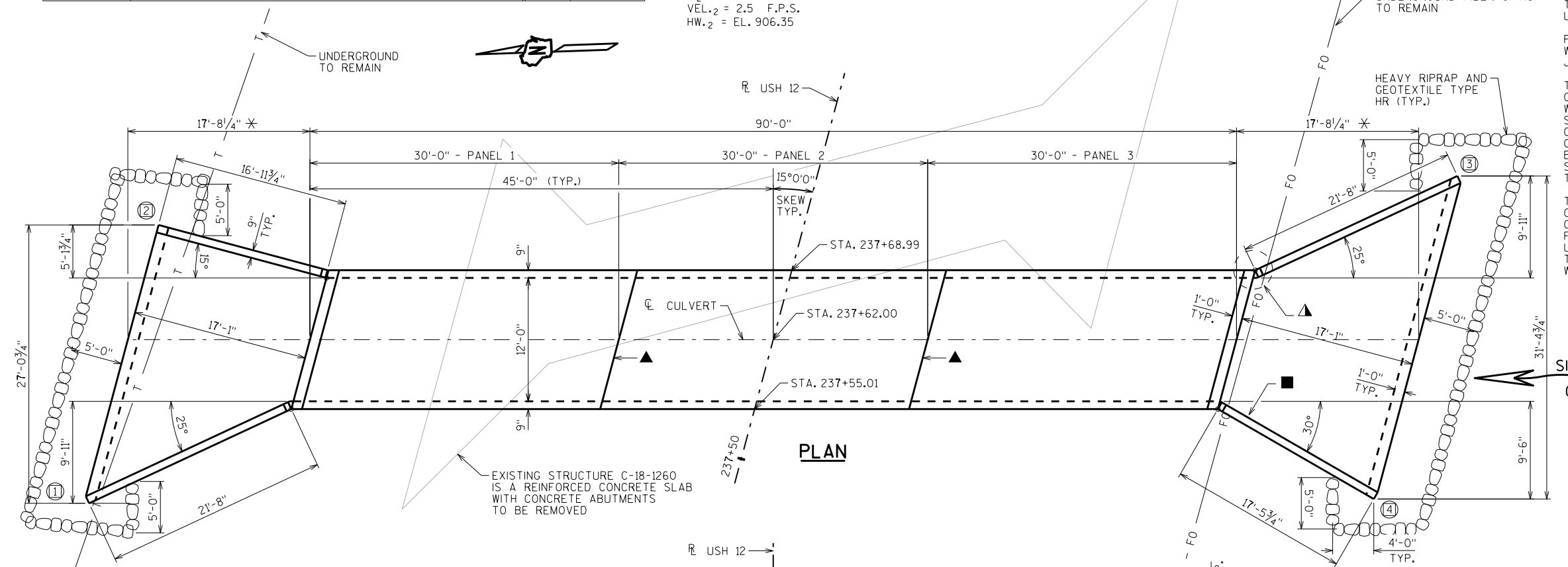
- DRAWINGS SHALL NOT BE SCALED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.
- THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
- BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.
- THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES CULVERTS C-18-49" SHALL BE THE EXISTING GROUNDLINE.
- ALL VOLUME WHICH CANNOT BE PLACED BEFORE CULVERT CONSTRUCTION AND NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL WITHIN THE LENGTH OF THE CULVERT INCLUDING THE APRON WING WALLS.
- THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.
- THE CONCRETE IN THE CUTOFF WALLS MAY BE PLACED UNDERWATER IF THE EXCAVATION CANNOT BE DEWATERED.
- PLACE 18" (MIN.) WIDE SHEET OF "RUBBERIZED MEMBRANE WATERPROOFING" ON TOP SLAB OVER ALL CONSTRUCTION JOINTS AND EXTEND DOWN TO BOTTOM OF OUTSIDE WALLS.
- THE CONTRACTOR MAY FURNISH A PRECAST CONCRETE BOX CULVERT IN LIEU OF THE CAST-IN-PLACE BOX CULVERT WITH THE ACCEPTANCE OF THE SHOP DRAWINGS BY THE STRUCTURES DESIGN SECTION. THE PRECAST CONCRETE BOX CULVERT SHALL CONFORM TO PRECAST DETAILS ON CHAPTER 36 STANDARDS OF THE CURRENT WISC. DOT BRIDGE MANUAL. PAYMENT FOR THE PRECAST CULVERT SHALL BE BASED ON THE QUANTITIES AND PRICES BID FOR THE ITEMS LISTED IN THE "TOTAL ESTIMATED QUANTITIES".
- THE CONTRACTOR MAY ELECT TO SUBSTITUTE #1 OR #2 CONCRETE COARSE AGGREGATE, SELECT CRUSHED MATERIAL OR OTHER GRANULAR MATERIAL AS APPROVED BY THE FIELD ENGINEER, IN LIEU OF THE BREAKER RUN, TO BE UTILIZED AS A CONSTRUCTION PLATFORM FOR THE BOX. THE CONTRACTOR IS RESPONSIBLE FOR BASE STABILITY WITH ANY SUBSTITUTED MATERIAL.

LIST OF DRAWINGS

1. LAYOUT
2. BOX DETAILS
3. APRON
4. APRON DETAILS
5. SUBSURFACE EXPLORATION

STRUCTURE DESIGN CONTACTS:

AHAM ALSKIF (608) 261-6113
 AARON BONK (608) 261-0261



8

8

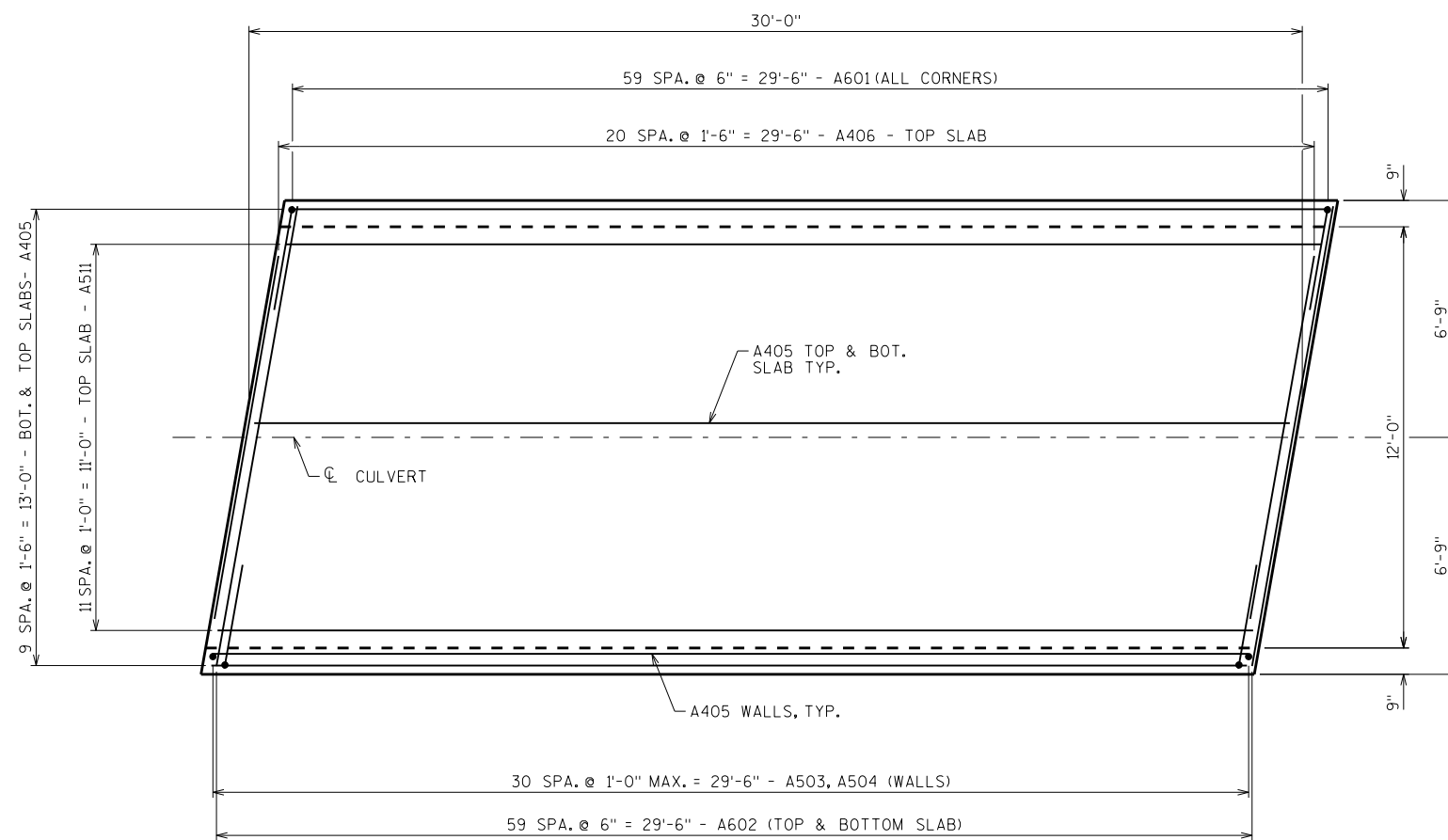
NO.	DATE	REVISION	BY
ACCEPTED		William C. Dehner CHIEF STRUCTURES DESIGN ENGINEER	
		12/20/19	
STRUCTURE C-18-49			
USH 12 OVER SIX MILE CREEK			
COUNTY	EAU CLAIRE	TOWN	WASHINGTON
DESIGN SPEC.	AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS		
DESIGNED BY	AA	DRAWN BY	MWB
LAYOUT			SHEET 1 OF 5

SCALE = 6.00

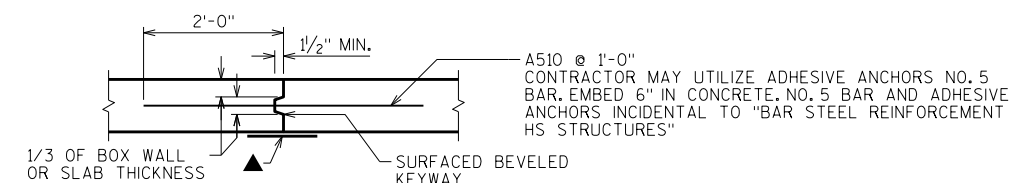
BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
A601		720	8'-6"	X		CORNERS
A602		360	13'-4"			TOP & BOT. SLAB - TRANS.
A503		186	2'-8"			WALLS - VERT. - DOWELS
A504		186	8'-0"			WALLS - VERT.
A405		120	29'-6"			TOP & BOT. SLAB & WALL LONGIT.
A406		63	10'-6"			TOP SLAB - TRANS.
A307		19	2'-8"	X		HEADER - VERT. - INLET
A308		19	2'-11"	X		HEADER - VERT. - OUTLET
A409		4	13'-4"			HEADER - HORIZ.
A510		122	4'-0"			VERT. CONST. JT. @ BOX APRON CONNECT.
A511		36	29'-6"			TOP SLAB LONGIT.

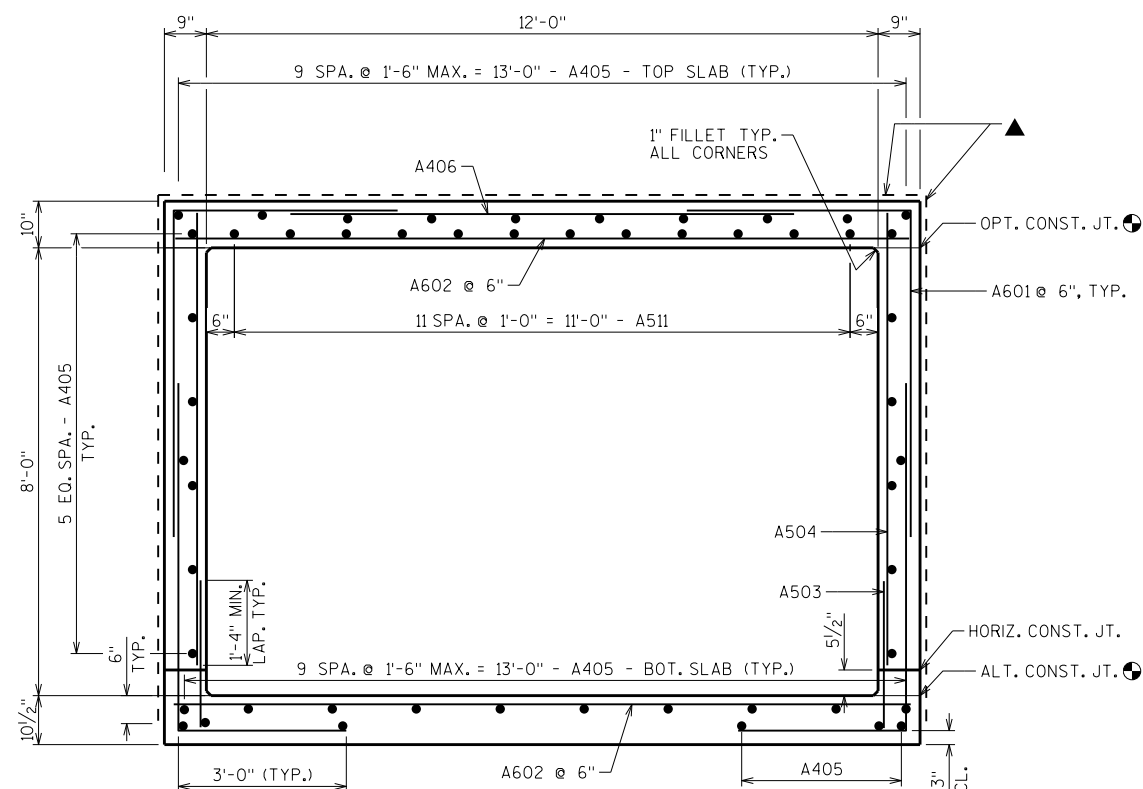


PLAN VIEW OF PANELS

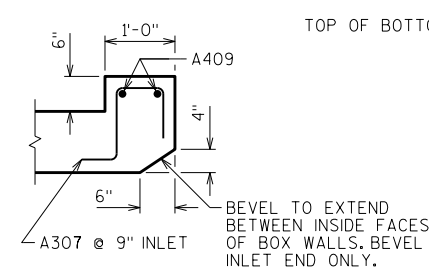


VERTICAL CONSTRUCTION JOINT

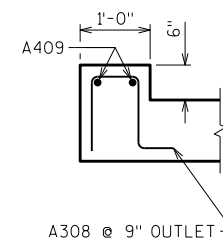
2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.



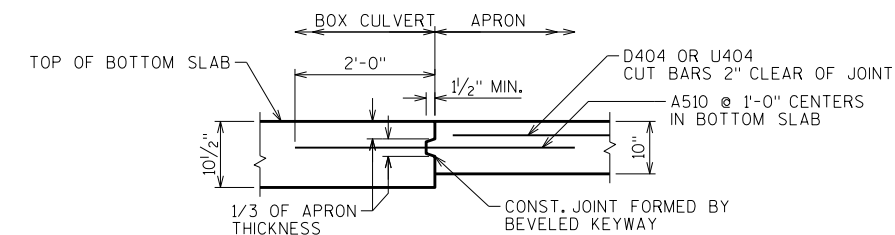
TYPICAL SECTION THRU PANELS



SECTION THRU INLET HEADER

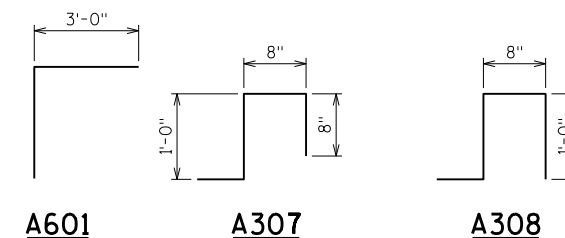


SECTION THRU OUTLET HEADER



APRON CONSTRUCTION JOINT

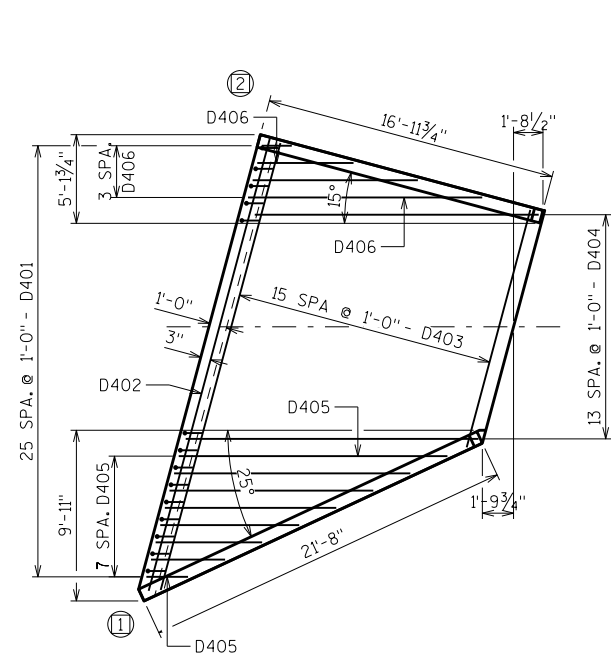
2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.



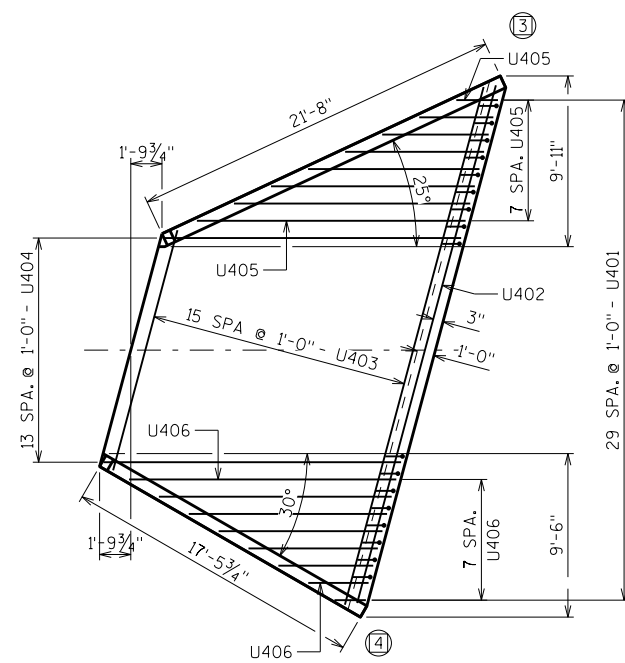
▲ 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING UP WALLS & ACROSS TOP SLAB AT VERTICAL CONSTRUCTION JOINTS. EXTEND 6" MIN. BELOW TOP OF BOTTOM SLAB.

⊕ OMIT 1" FILLET IF ALT. CONST. JT. USED

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-18-49			
DRAWN BY AA		PLANS CK'D. MWB	
BOX DETAILS		SHEET 2	



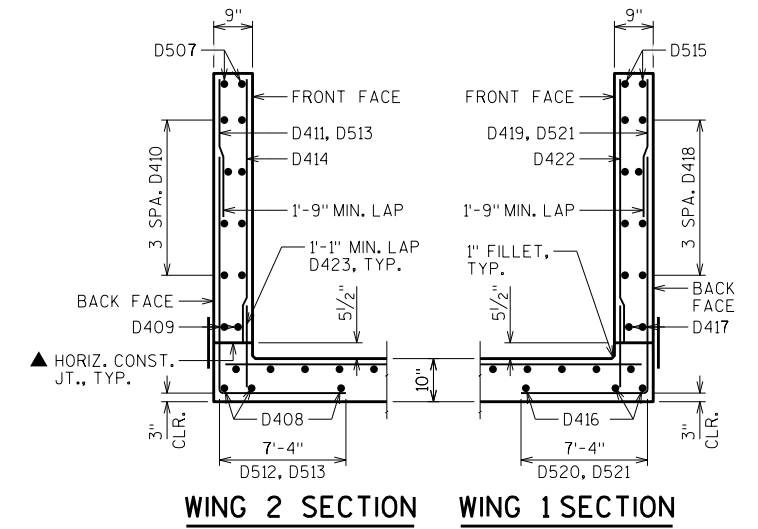
OUTLET APRON PLAN



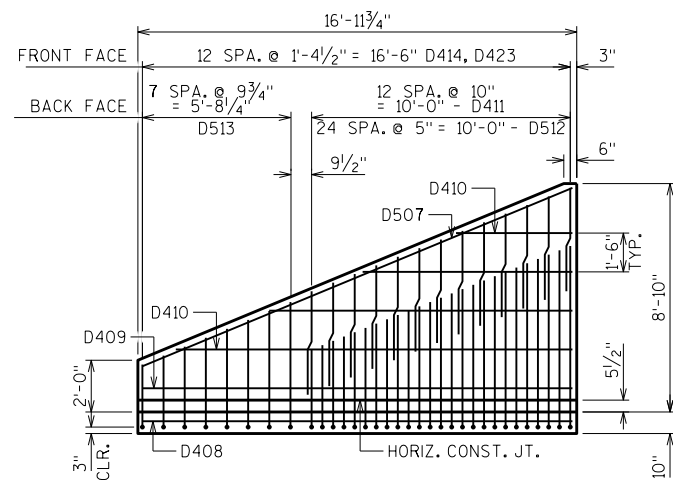
INLET APRON PLAN

Ⓢ INDICATES WING NUMBER

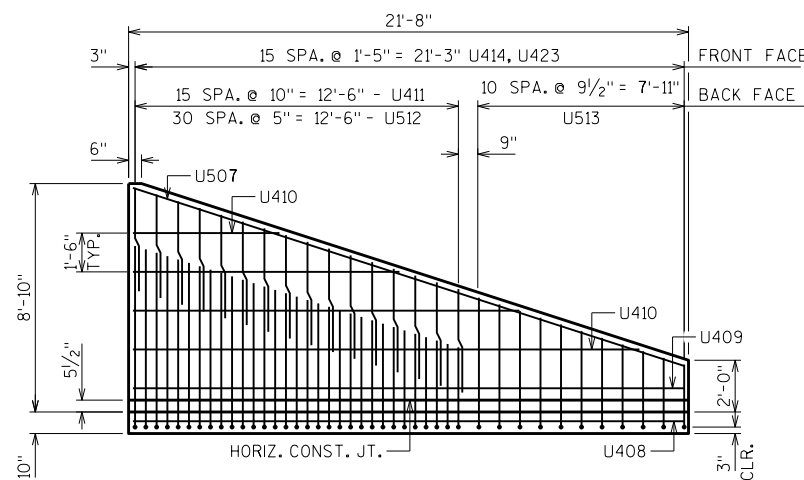
▲ 18" RUBBERIZED MEMBRANE WATERPROOFING, PLACE ALONG HORIZ. CONST. JT. FOR ENTIRE WING LENGTH, TYP.



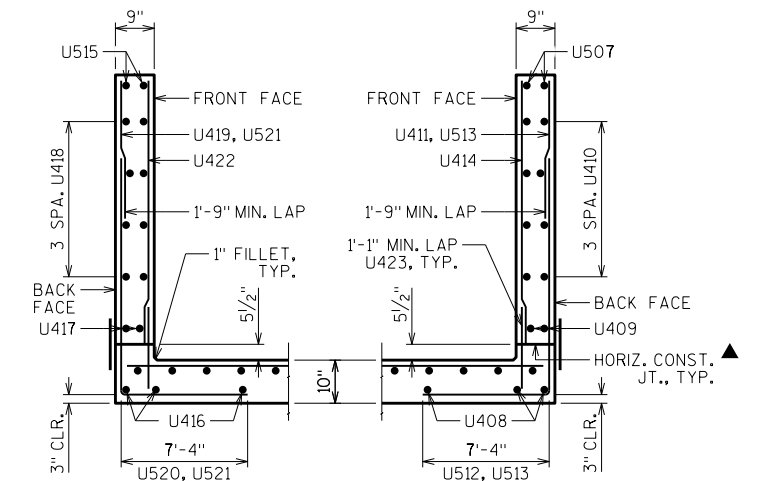
WING 2 SECTION WING 1 SECTION



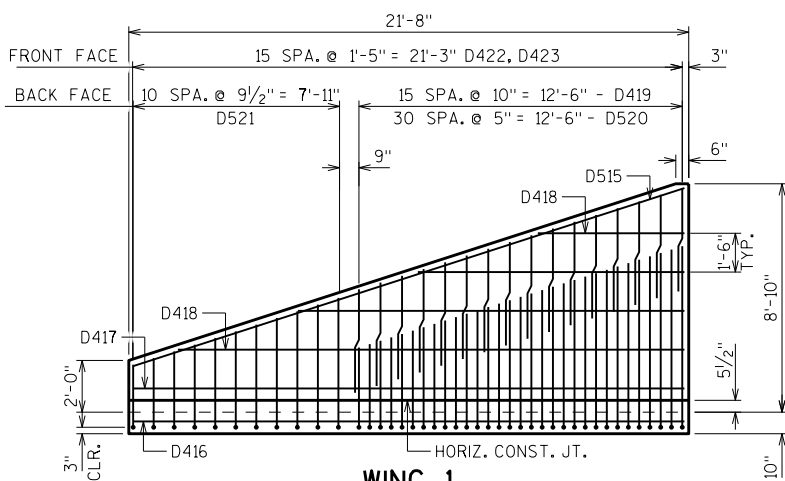
WING 2



WING 3



WING 4 SECTION WING 3 SECTION



BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
D401		26	5'-6"	X		OUTLET APRON AND CUTOFF WALL VERT.
D402		5	27'-2"			OUTLET APRON AND CUTOFF WALL HORIZ.
D403		16	20'-7"		▲	OUTLET APRON SLAB HORIZ.
D404		14	17'-3"			OUTLET APRON HORIZ.
D405		8	9'-0"		▲	OUTLET APRON SLAB HORIZ.
D406		4	7'-9"		▲	OUTLET APRON SLAB HORIZ.
D507	X	2	17'-9"			WING 2 HORIZ. TOP BOTH FACES
D408	X	3	16'-6"			WING 2 HORIZ. APRON SLAB
D409	X	2	16'-6"			WING 2 HORIZ. BOTTOM BOTH FACES
D410	X	8	9'-11"		▲	WING 2 HORIZ.
D411	X	13	4'-0"			WING 2 VERT. BACK FACE
D512	X	25	12'-3"	X	▲	WING 2 VERT. BACK FACE
D513	X	8	10'-10"	X	▲	WING 2 VERT. BACK FACE
D414	X	13	4'-9"		▲	WING 2 VERT. FRONT FACE
D515	X	2	22'-2"			WING 1 HORIZ. TOP BOTH FACES
D416	X	3	21'-2"			WING 1 HORIZ. APRON SLAB
D417	X	2	21'-2"			WING 1 HORIZ. BOTTOM BOTH FACES
D418	X	8	12'-8"		▲	WING 1 HORIZ.
D419	X	16	4'-0"			WING 1 VERT. BACK FACE
D520	X	31	12'-3"	X	▲	WING 1 VERT. BACK FACE
D521	X	11	10'-11"	X	▲	WING 1 VERT. BACK FACE
D422	X	16	4'-9"		▲	WING 1 VERT. FRONT FACE
D423	X	29	2'-5"			WINGS 1 & 2 DOWELS FRONT FACE
U401		30	3'-6"	X		INLET APRON AND CUTOFF WALL VERT.
U402		3	31'-1"			INLET APRON AND CUTOFF WALL HORIZ.
U403		16	22'-9"		▲	INLET APRON SLAB HORIZ.
U404		14	17'-3"			INLET APRON HORIZ.
U405		8	9'-0"		▲	INLET APRON SLAB HORIZ.
U406		8	8'-7"		▲	INLET APRON SLAB HORIZ.
U507	X	2	22'-2"			WING 3 HORIZ. TOP BOTH FACES
U408	X	3	21'-2"			WING 3 HORIZ. APRON SLAB
U409	X	2	21'-2"			WING 3 HORIZ. BOTTOM BOTH FACES
U410	X	8	12'-8"		▲	WING 3 HORIZ.
U411	X	16	4'-0"			WING 3 VERT. BACK FACE
U512	X	31	12'-3"	X	▲	WING 3 VERT. BACK FACE
U513	X	11	10'-11"	X	▲	WING 3 VERT. BACK FACE
U414	X	16	4'-9"		▲	WING 3 VERT. FRONT FACE
U515	X	2	18'-3"			WING 4 HORIZ. TOP BOTH FACES
U416	X	3	17'-0"			WING 4 HORIZ. APRON SLAB
U417	X	2	17'-0"			WING 4 HORIZ. BOTTOM BOTH FACES
U418	X	8	10'-2"		▲	WING 4 HORIZ.
U419	X	13	4'-0"			WING 4 VERT. BACK FACE
U520	X	25	12'-3"	X	▲	WING 4 VERT. BACK FACE
U521	X	9	10'-10"	X	▲	WING 4 VERT. BACK FACE
U422	X	13	4'-9"		▲	WING 4 VERT. FRONT FACE
U423	X	29	2'-5"			WINGS 3 & 4 DOWELS FRONT FACE

▲ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

BAR SERIES TABLE

BAR MARK	NO. REQ'D	LENGTH
D403	1 SERIES OF 16	14'-3" TO 26'-11"
D405	1 SERIES OF 8	2'-5" TO 15'-7"
D406	1 SERIES OF 4	1'-9" TO 13'-9"
D410	2 SERIES OF 4	4'-6" TO 15'-4"
D512	1 SERIES OF 25	10'-3" TO 14'-3"
D513	1 SERIES OF 8	9'-7" TO 12'-0"
D414	1 SERIES OF 13	1'-5" TO 8'-2"
D418	2 SERIES OF 4	5'-8" TO 19'-7"
D520	1 SERIES OF 31	10'-3" TO 14'-2"
D521	1 SERIES OF 11	9'-7" TO 12'-2"
D422	1 SERIES OF 16	1'-5" TO 8'-2"
U403	1 SERIES OF 16	14'-5" TO 31'-1"
U405	1 SERIES OF 8	2'-5" TO 15'-7"
U406	1 SERIES OF 8	1'-8" TO 15'-6"
U410	2 SERIES OF 4	5'-8" TO 19'-7"
U512	1 SERIES OF 31	10'-3" TO 14'-3"
U513	1 SERIES OF 11	9'-7" TO 12'-2"
U414	1 SERIES OF 16	1'-5" TO 8'-2"
U418	2 SERIES OF 4	4'-7" TO 15'-9"
U520	1 SERIES OF 25	10'-3" TO 14'-3"
U521	1 SERIES OF 9	9'-7" TO 12'-1"
U422	1 SERIES OF 13	1'-5" TO 8'-2"

BUNDLE AND TAG EACH SERIES SEPERATELY

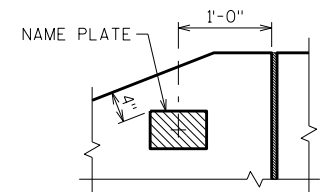
○ 3/4" FILLER, TYP. EXTEND FILLER FROM HORIZ. CONST. JT. TO TOP OF WING.

■ 1" BEVEL, TYP.

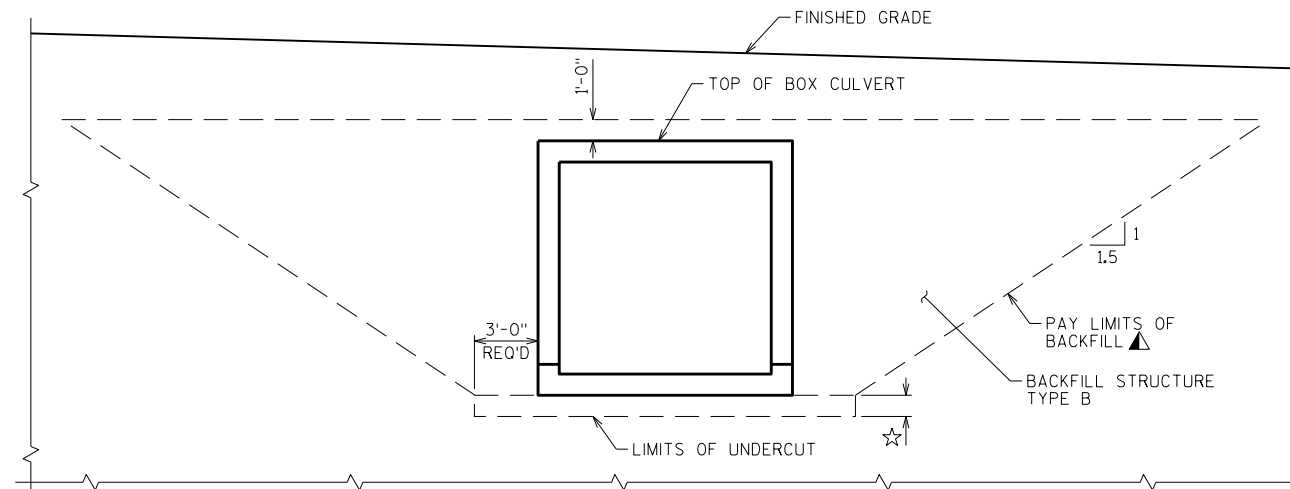
▲ 18" RUBBERIZED MEMBRANE WATERPROOFING. EXTEND FROM HORIZ. CONST. JT. TO TOP OF WALL.

STATE PROJECT NUMBER

7080-01-72



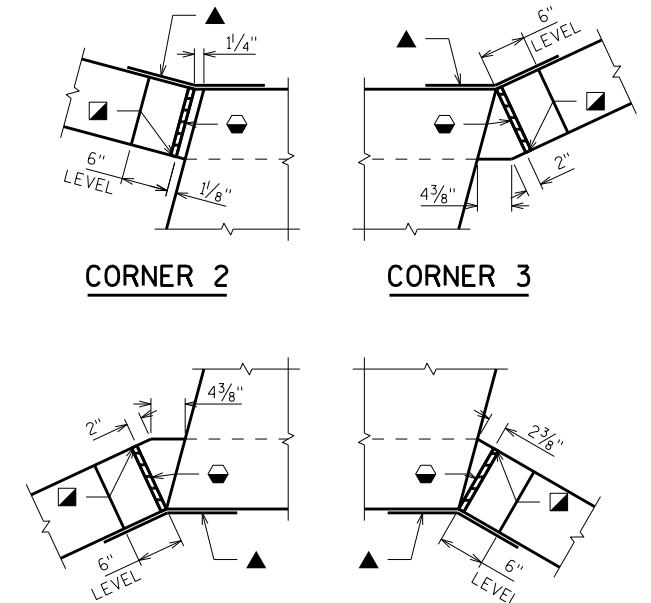
**NAME PLATE LOCATION
WING 4**



**TYPICAL SECTION
THRU BOX CULVERT**

▲ BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

☆ UNDER CUT 1'-0". EXCAVATION FOR UNDER CUT TO BE INCLUDED IN EXCAVATION FOR STRUCTURES. PLACE "GEOTEXTILE TYPE C" AND BACKFILL WITH "BREAKER RUN".



CORNER 2

CORNER 3

CORNER 1

CORNER 4

CORNER DETAILS

**U512, U513, U520, U521,
D512, D513, D520, D521**

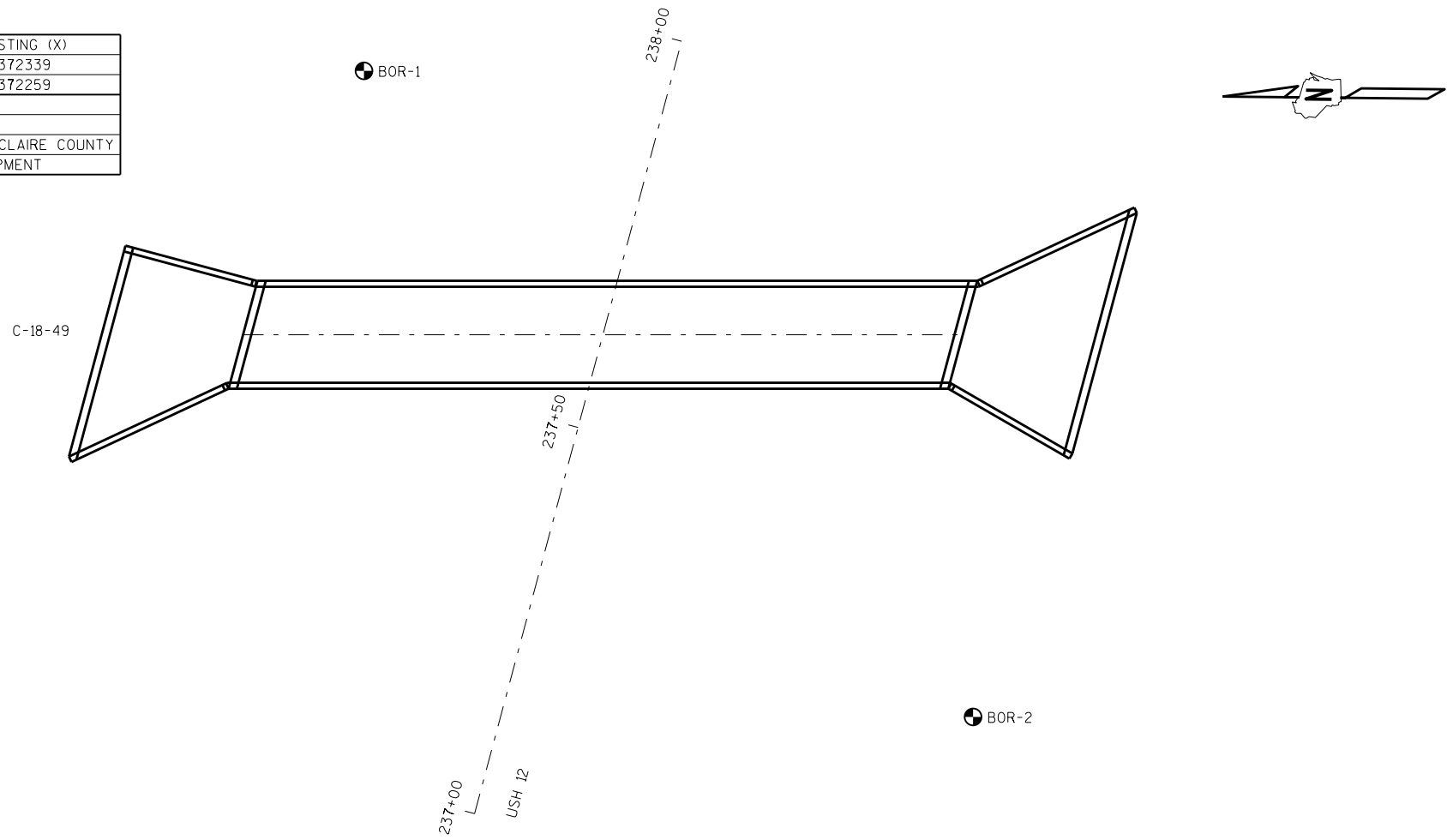
U401, D401

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-18-49			
DRAWN BY		AA	PLANS CK'D. MWB
APRON DETAILS			SHEET 4

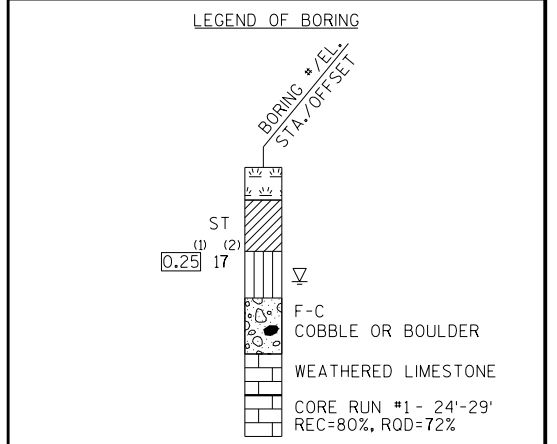
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BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	3/19/2019	269662	372339
2	3/20/2019	269585	372259

BORINGS COMPLETED BY: WISDOT
 REPORT COMPLETED BY: WISDOT
 ALL COORDINATES REFERENCED TO WCCS NAD 83(91) EAU CLAIRE COUNTY
 COORDINATES COLLECTED USING NON-SURVEY GRADE EQUIPMENT



STATE PROJECT NUMBER		
7080-01-72		
MATERIAL SYMBOLS		
	ASPHALT	
	CONCRETE	
	SAND	
	BOULDERS OR COBBLES	
	SHALE	
	SANDSTONE	
	LIMESTONE	
	IGNEOUS/META	



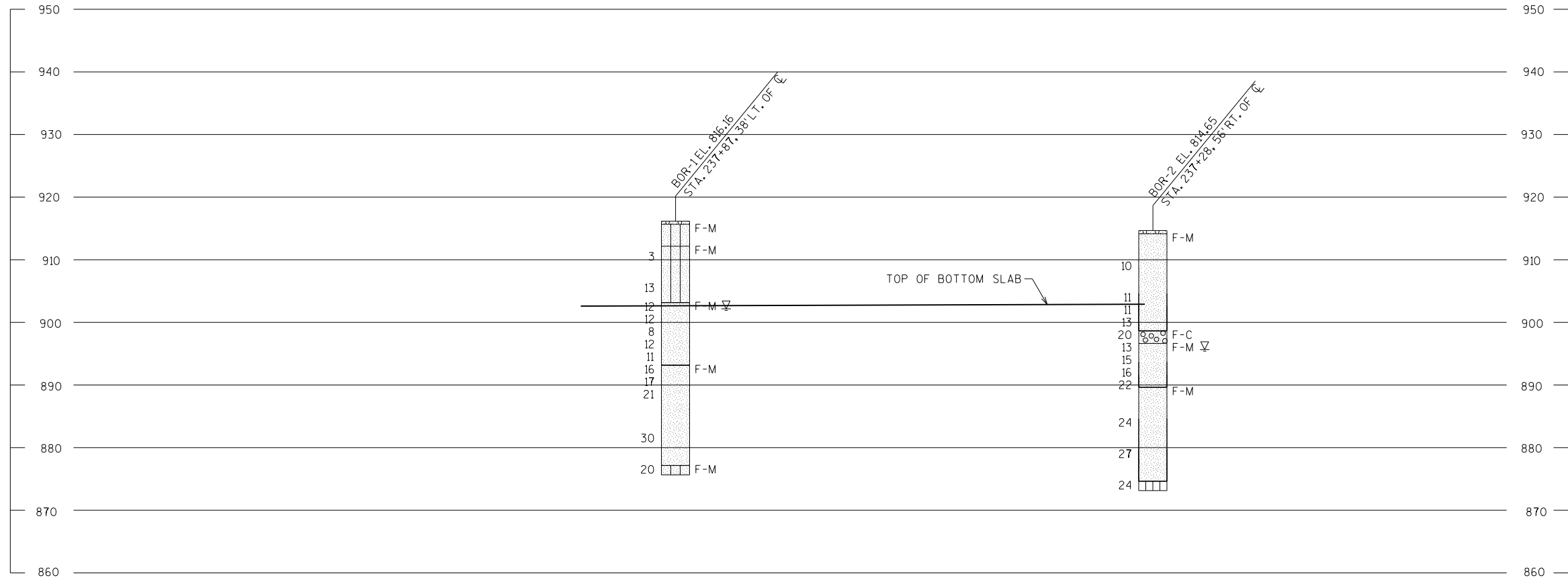
(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)
 (2) UNLESS OTHERWISE SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION
 ▽ AT TIME OF DRILLING
 ▽ END OF DRILLING
 ▽ AFTER DRILLING

ABBREVIATIONS
 F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.



NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE C-18-49

DRAWN BY: TLP/AA PLANS CKD. **MWB**

SUBSURFACE EXPLORATION

SHEET 5

8

8

SCALE = 10

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
203.0200	REMOVING OLD STRUCTURE STA. 347+00	LS	1
206.2000	EXCAVATION FOR STRUCTURES CULVERTS C-18-50	LS	1
210.2500	BACKFILL STRUCTURE TYPE B	TON	1,285
311.0115	BREAKER RUN	CY	192
504.0100	CONCRETE MASONRY CULVERTS	CY	158
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	28,870
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	3,470
511.1200	TEMPORARY SHORING C-18-50	SF	390
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	27
606.0300	RIPRAP HEAVY	CY	70
645.0105	GEOTEXTILE TYPE C	SY	324
645.0120	GEOTEXTILE TYPE HR	SY	161
NON-BID ITEMS			
	FILLER	SIZE	¾"

- ▲ SEE CORNER DETAILS ON "APRON DETAILS" SHEET
- NAME PLATE LOCATION (SEE "APRON DETAILS" SHEET)
- * BUILD APRON AND END OF BOX LEVEL
- INDICATES WING NUMBER
- ▲ VERT. CONST. JOINT (TYP.)

NOTE: STRUCTURE BACKFILL REQUIRED BEHIND ALL WING WALLS

STATE PROJECT NUMBER

7080-01-72

GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.
- THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
- BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.
- THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES CULVERTS C-18-50" SHALL BE THE EXISTING GROUNDLINE.
- ALL VOLUME WHICH CANNOT BE PLACED BEFORE CULVERT CONSTRUCTION AND NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL WITHIN THE LENGTH OF THE CULVERT INCLUDING THE APRON WING WALLS.
- THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.
- THE CONCRETE IN THE CUTOFF WALLS MAY BE PLACED UNDERWATER IF THE EXCAVATION CANNOT BE DEWATERED.
- PLACE 18" (MIN.) WIDE SHEET OF "RUBBERIZED MEMBRANE WATERPROOFING" ON TOP SLAB OVER ALL CONSTRUCTION JOINTS AND EXTEND DOWN TO BOTTOM OF OUTSIDE WALLS.
- THE CONTRACTOR MAY FURNISH A PRECAST CONCRETE BOX CULVERT IN LIEU OF THE CAST-IN-PLACE BOX CULVERT WITH THE ACCEPTANCE OF THE SHOP DRAWINGS BY THE STRUCTURES DESIGN SECTION. THE PRECAST CONCRETE BOX CULVERT SHALL CONFORM TO PRECAST DETAILS ON CHAPTER 36 STANDARDS OF THE CURRENT WISC. DOT BRIDGE MANUAL. PAYMENT FOR THE PRECAST CULVERT SHALL BE BASED ON THE QUANTITIES AND PRICES BID FOR THE ITEMS LISTED IN THE "TOTAL ESTIMATED QUANTITIES".
- THE CONTRACTOR MAY ELECT TO SUBSTITUTE #1 OR #2 CONCRETE COARSE AGGREGATE, SELECT CRUSHED MATERIAL OR OTHER GRANULAR MATERIAL AS APPROVED BY THE FIELD ENGINEER, IN LIEU OF THE BREAKER RUN, TO BE UTILIZED AS A CONSTRUCTION PLATFORM FOR THE BOX. THE CONTRACTOR IS RESPONSIBLE FOR BASE STABILITY WITH ANY SUBSTITUTED MATERIAL.

DESIGN DATA

LIVE LOAD:
 DESIGN LOADING: HL-93
 INVENTORY RATING FACTOR: RF = 1.05
 OPERATING RATING FACTOR: RF = 1.35
 WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 190(KIPS)

EARTHLOAD:
 DESIGNED FOR 0.5 TO 2 FT. OF FILL.

MATERIAL PROPERTIES:
 CONCRETE MASONRY: f'c = 3,500 P.S.I.
 BAR STEEL REINFORCEMENT: fy = 60,000 P.S.I.

HYDRAULIC DATA

100 YEAR FREQUENCY
 Q₁₀₀ = 450 C.F.S.
 VEL₁₀₀ = 4.71 F.P.S.
 HW₁₀₀ = EL. 934.94
 WATERWAY AREA = 112 SQ. FT.
 DRAINAGE AREA = 3.0 SQ. MI.
 ROADWAY OVERTOPPING = N/A
 SCOUR CRITICAL CODE = 8

TRAFFIC VOLUME

USH 12
 ADT = 7,700 (2040)
 R.D.S. = 60 M.P.H.



2 YEAR FREQUENCY

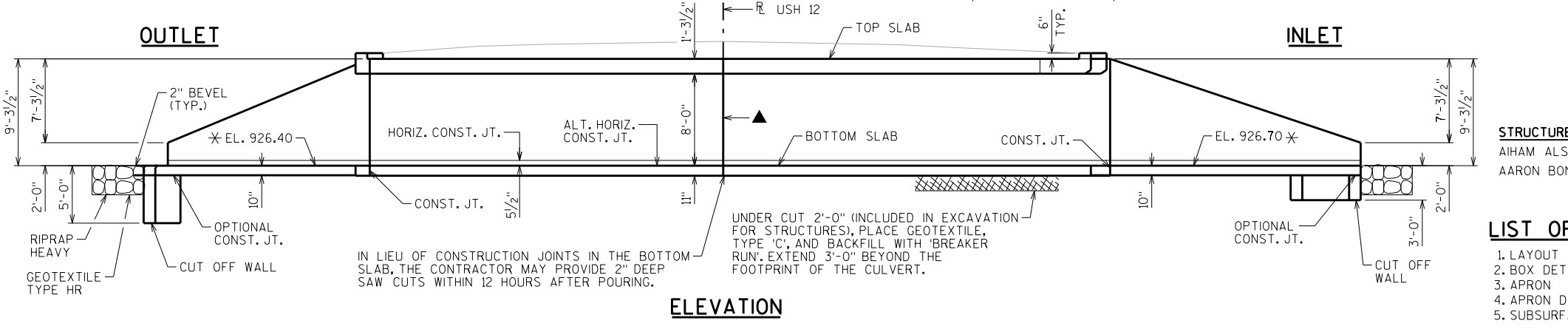
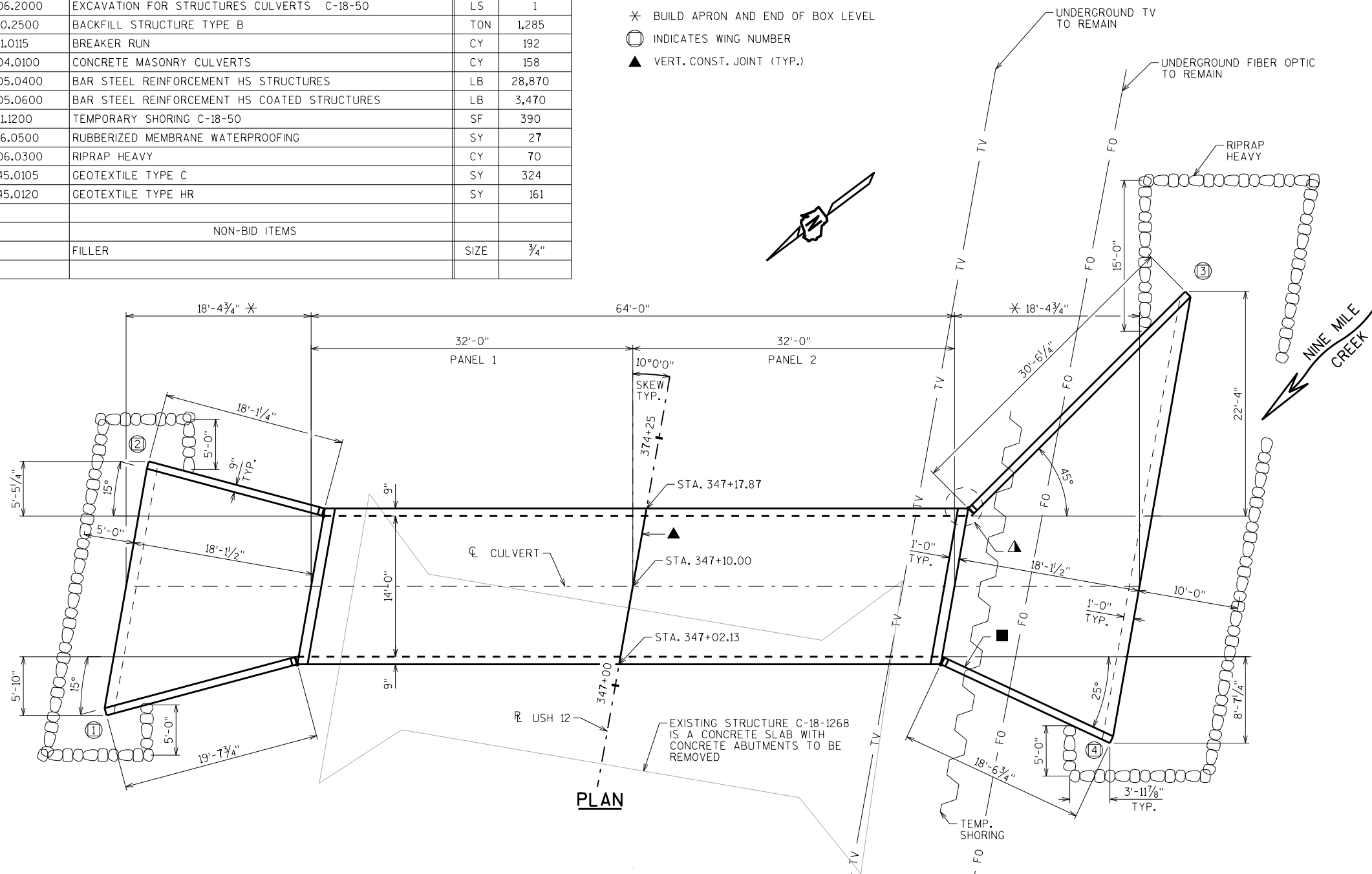
Q₂ = 105 C.F.S.
 VEL₂ = 1.88 F.P.S.
 HW₂ = EL. 932.05

STRUCTURE DESIGN CONTACTS:
 AIHAM ALSKIF (608) 261-6113
 AARON BONK (608) 261-0261

LIST OF DRAWINGS

1. LAYOUT
2. BOX DETAILS
3. APRON
4. APRON DETAILS
5. SUBSURFACE EXPLORATION

NO.	DATE	REVISION	BY
			
ACCEPTED		 CHIEF STRUCTURES DESIGN ENGINEER	
		12/20/19	
STRUCTURE C-18-50			
USH 12 OVER NINE MILE CREEK			
COUNTY	EAU CLAIRE	TOWN	WASHINGTON
DESIGN SPEC.	AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS		
DESIGNED BY	AA	DESIGNED CK'D.	MWB
LAYOUT		SHEET 1 OF 5	



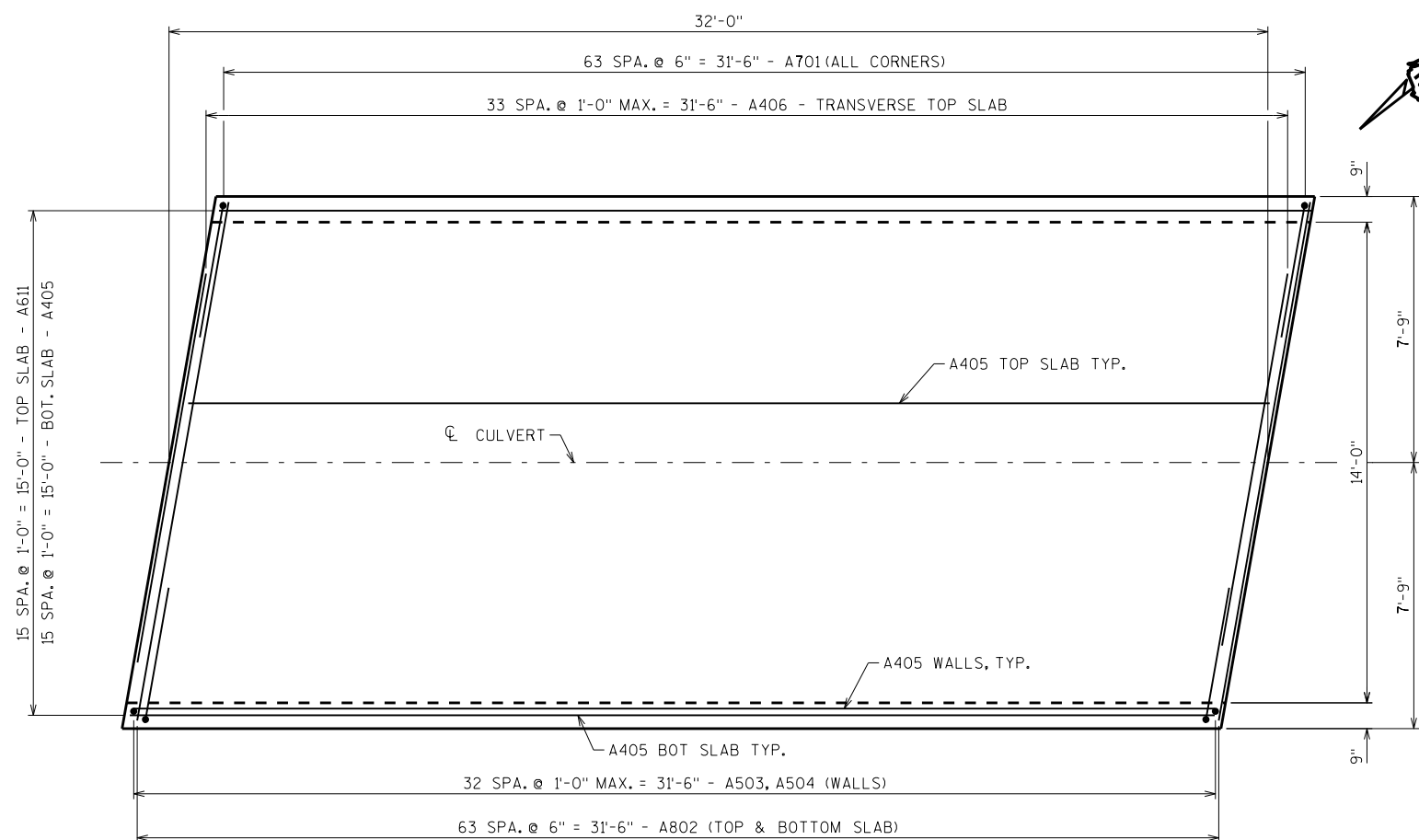
8

8

BILL OF BARS

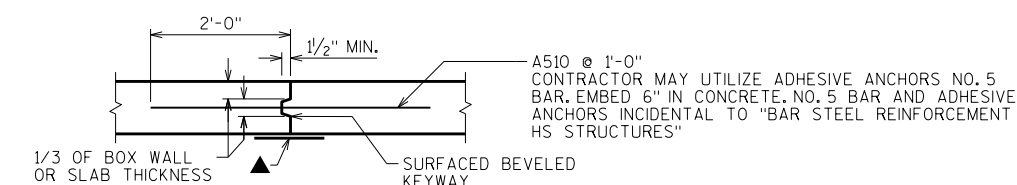
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
A701		512	10'-2"	X		CORNERS
A802		256	15'-4"			TOP & BOT. SLAB - TRANS.
A503		132	2'-8"			WALLS - VERT. - DOWELS
A504		132	8'-6"			WALLS - VERT.
A405		104	31'-8"			TOP & BOT. SLAB & WALL LONGIT.
A406		66	11'-6"			TOP SLAB - TRANS.
A307		22	3'-6"	X		HEADER - VERT. - INLET
A308		22	3'-9"	X		HEADER - VERT. - OUTLET
A409		4	15'-4"			HEADER - HORIZ.
A510		128	4'-0"			VERT. CONST. JT. @ BOX APRON CONNECT.
A611		28	31'-8"			TOP SLAB LONGIT.



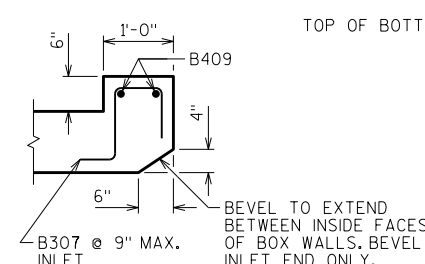
PLAN VIEW OF PANELS

SOUTH PANEL SHOWN, NORTH PANEL SIMILAR

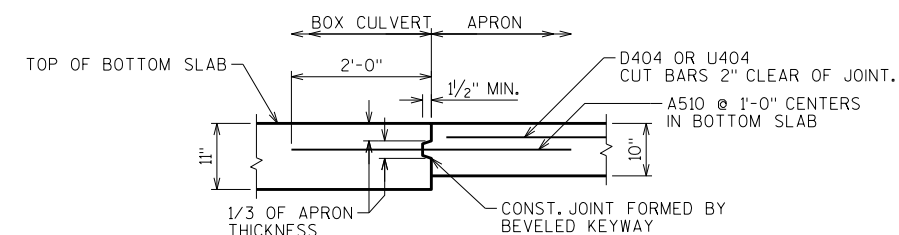


VERTICAL CONSTRUCTION JOINT

2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.

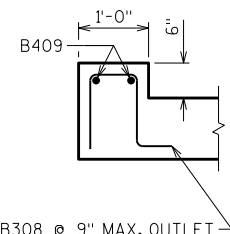


SECTION THRU INLET HEADER

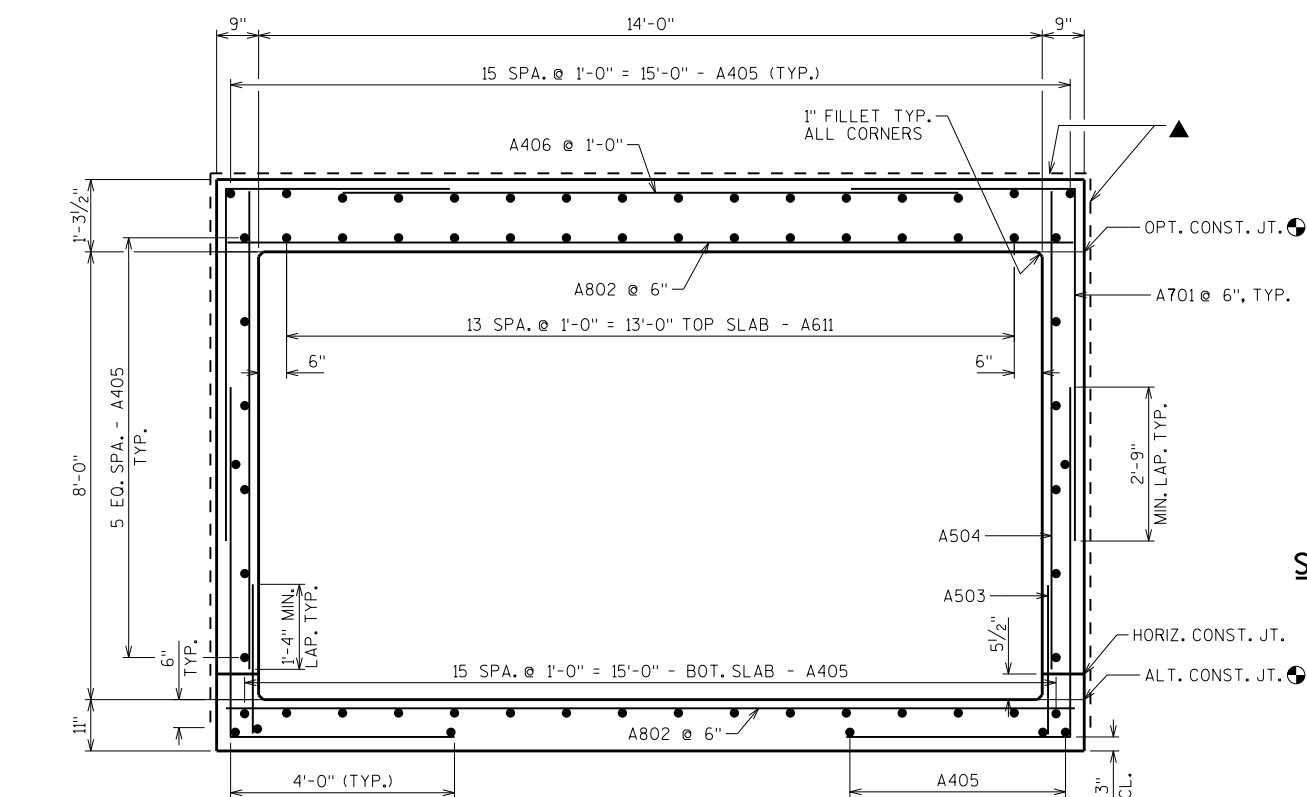
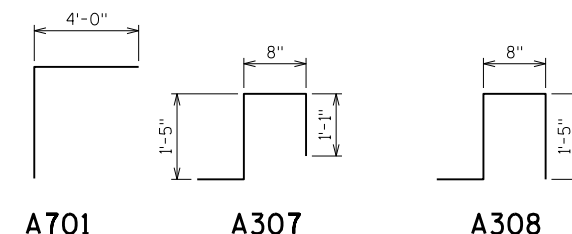


APRON CONSTRUCTION JOINT

2" DEEP SAW CUT WITHIN 12 HOURS AFTER POURING MAY BE USED IN LIEU OF CONST. JT. IN BOTTOM SLAB.



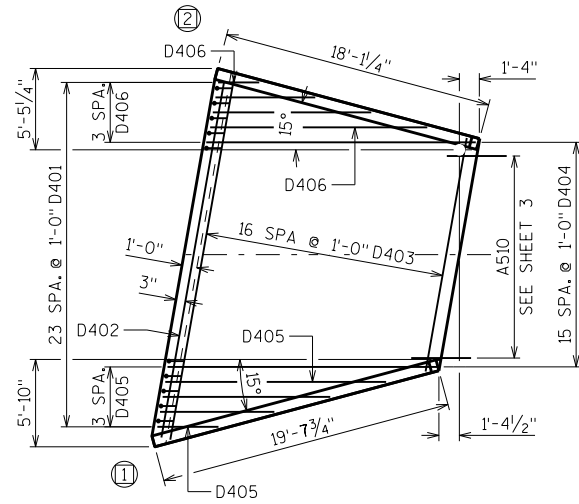
SECTION THRU OUTLET HEADER



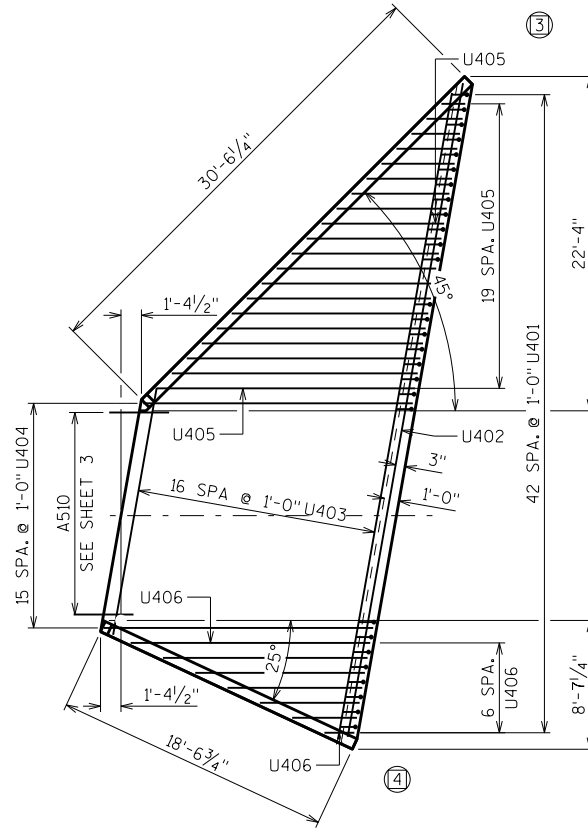
TYPICAL SECTION THRU PANELS

- ▲ 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING UP WALLS & ACROSS TOP SLAB AT VERTICAL CONSTRUCTION JOINTS. EXTEND 6" MIN. BELOW TOP OF BOTTOM SLAB.
- ⊕ OMIT 1" FILLET IF ALT CONST. JT. USED

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-18-50			
DRAWN BY AA		PLANS CKD. MWB	
BOX DETAILS		SHEET 2	

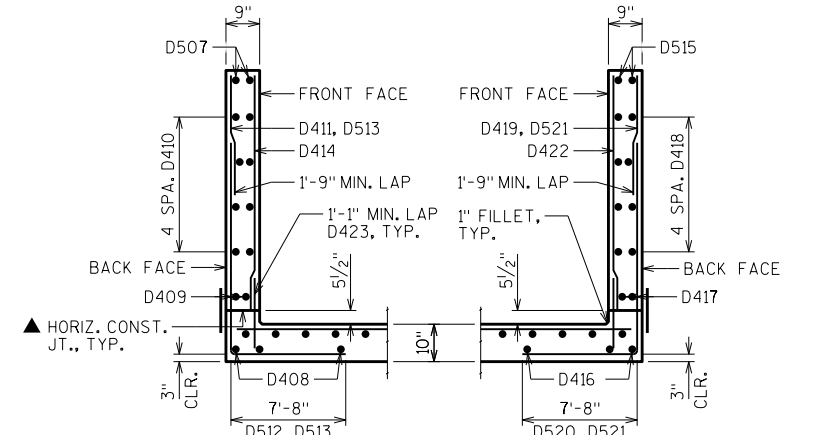


OUTLET APRON PLAN

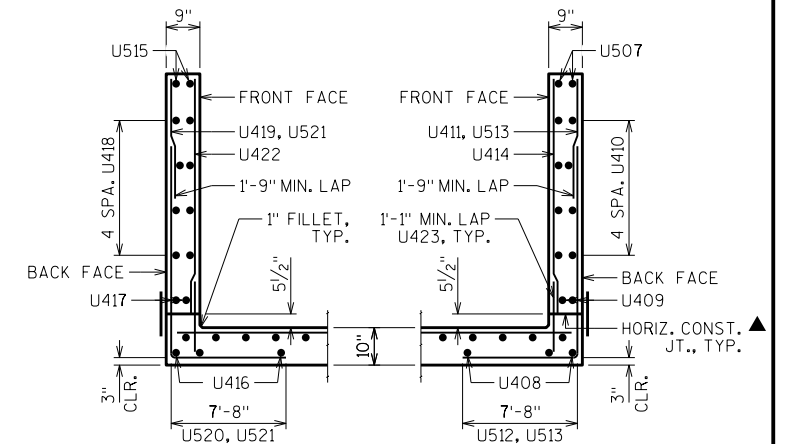


INLET APRON PLAN

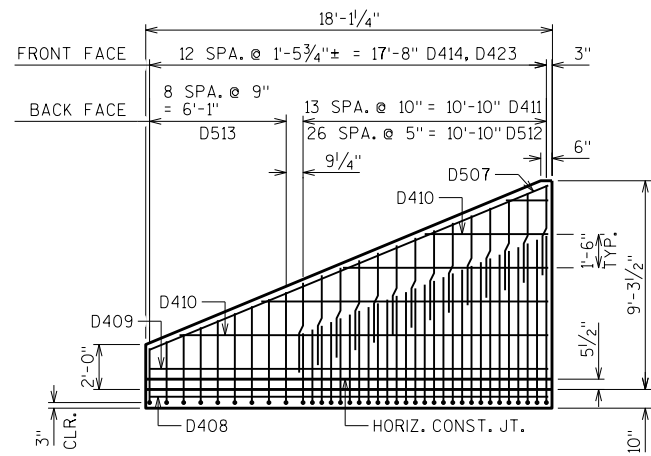
▲ 18" RUBBERIZED MEMBRANE WATERPROOFING, PLACE ALONG HORIZ. CONST. JT. FOR ENTIRE WING LENGTH, TYP.
 ○ INDICATES WING NUMBER



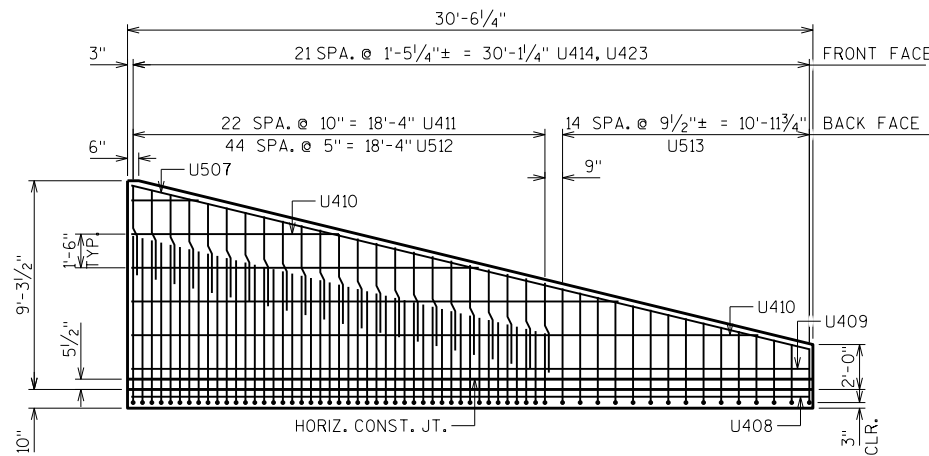
WING 2 SECTION WING 1 SECTION



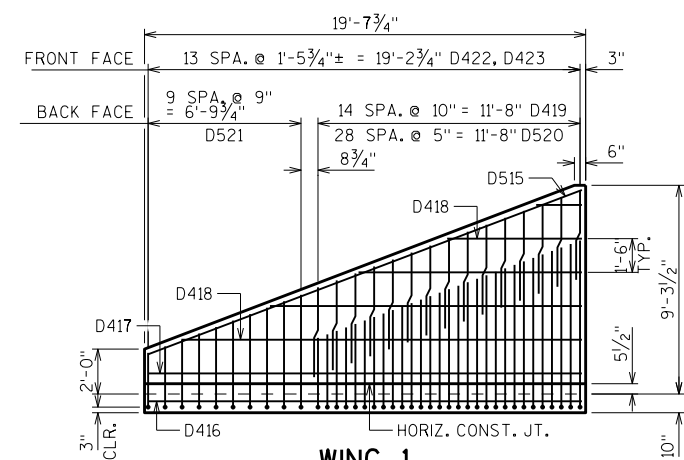
WING 4 SECTION WING 3 SECTION



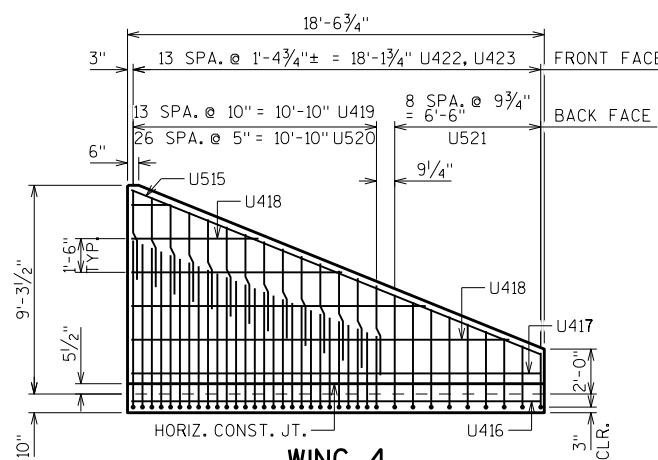
WING 2



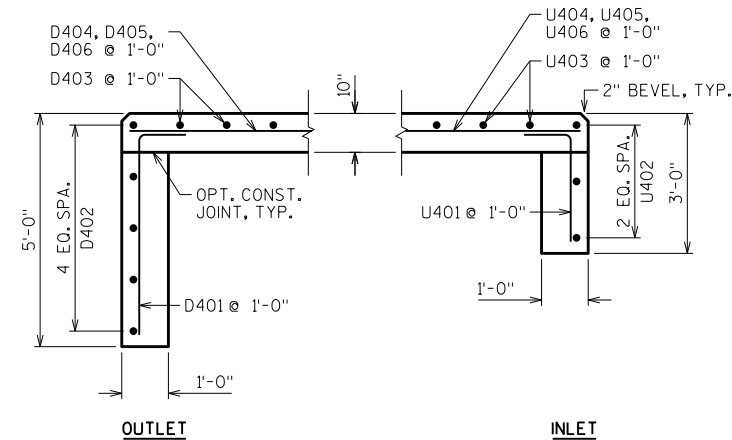
WING 3



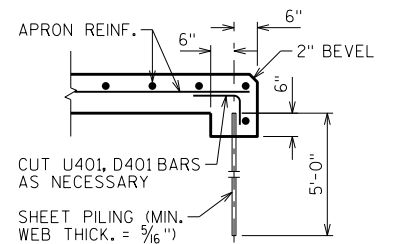
WING 1



WING 4



CUT-OFF WALLS



ALTERNATE CUT-OFF WALLS

THE ABOVE ALTERNATIVE MAY BE USED IN LIEU OF CAST-IN-PLACE CONCRETE CUT-OFF WALLS. PAYMENT WILL BE BASED ON THE CONCRETE CUT-OFF WALLS.

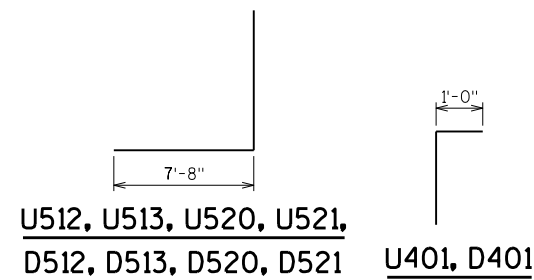
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-18-50			
DRAWN BY AA		PLANS CK'D. MWB	
APRON		SHEET 3	

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	COAT	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
D401		24	5'-6"	X		OUTLET APRON AND CUTOFF WALL VERT.
D402		5	24'-5"			OUTLET APRON AND CUTOFF WALL HORIZ.
D403		17	20'-3"		▲	OUTLET APRON SLAB HORIZ.
D404		16	18'-0"			OUTLET APRON SLAB HORIZ.
D405		4	9'-5"		▲	OUTLET APRON SLAB HORIZ.
D406		4	8'-8"		▲	OUTLET APRON SLAB HORIZ.
D507	X	2	19'-0"			WING 2 HORIZ. TOP BOTH FACES
D408	X	3	17'-8"			WING 2 HORIZ. APRON SLAB
D409	X	2	17'-8"			WING 2 HORIZ. BOTTOM BOTH FACES
D410	X	10	9'-1"		▲	WING 2 HORIZ.
D411	X	14	4'-0"			WING 2 VERT. BACK FACE
D512	X	27	12'-10"	X	▲	WING 2 VERT. BACK FACE
D513	X	9	11'-2"	X	▲	WING 2 VERT. BACK FACE
D414	X	13	5'-0"		▲	WING 2 VERT. FRONT FACE
D515	X	2	20'-5"			WING 1 HORIZ. TOP BOTH FACES
D416	X	3	19'-2"			WING 1 HORIZ. APRON SLAB
D417	X	2	19'-2"			WING 1 HORIZ. BOTTOM BOTH FACES
D418	X	10	9'-11"		▲	WING 1 HORIZ.
D419	X	15	4'-0"			WING 1 VERT. BACK FACE
D520	X	29	12'-10"	X	▲	WING 1 VERT. BACK FACE
D521	X	10	11'-2"	X	▲	WING 1 VERT. BACK FACE
D422	X	14	5'-0"		▲	WING 1 VERT. FRONT FACE
D423	X	27	2'-5"			WINGS 1 & 2 DOWELS FRONT FACE
U401		43	3'-6"	X		INLET APRON AND CUTOFF WALL VERT.
U402		3	44'-4"			INLET APRON AND CUTOFF WALL HORIZ.
U403		17	30'-4"		▲	INLET APRON SLAB HORIZ.
U404		16	18'-0"			INLET APRON SLAB HORIZ.
U405		20	9'-6"		▲	INLET APRON SLAB HORIZ.
U406		7	9'-0"		▲	INLET APRON SLAB HORIZ.
U507	X	2	30'-10"			WING 3 HORIZ. TOP BOTH FACES
U408	X	3	30'-1"			WING 3 HORIZ. APRON SLAB
U409	X	2	30'-1"			WING 3 HORIZ. BOTTOM BOTH FACES
U410	X	10	15'-5"		▲	WING 3 HORIZ.
U411	X	23	4'-0"			WING 3 VERT. BACK FACE
U512	X	45	12'-10"	X	▲	WING 3 VERT. BACK FACE
U513	X	15	11'-3"	X	▲	WING 3 VERT. BACK FACE
U414	X	22	5'-0"		▲	WING 3 VERT. FRONT FACE
U515	X	2	19'-5"			WING 4 HORIZ. TOP BOTH FACES
U416	X	3	18'-1"			WING 4 HORIZ. APRON SLAB
U417	X	2	18'-1"			WING 4 HORIZ. BOTTOM BOTH FACES
U418	X	10	9'-4"		▲	WING 4 HORIZ.
U419	X	14	4'-0"			WING 4 VERT. BACK FACE
U520	X	27	12'-10"	X	▲	WING 4 VERT. BACK FACE
U521	X	9	11'-2"	X	▲	WING 4 VERT. BACK FACE
U422	X	14	5'-0"		▲	WING 4 VERT. FRONT FACE
U423	X	36	2'-5"			WINGS 3 & 4 DOWELS FRONT FACE

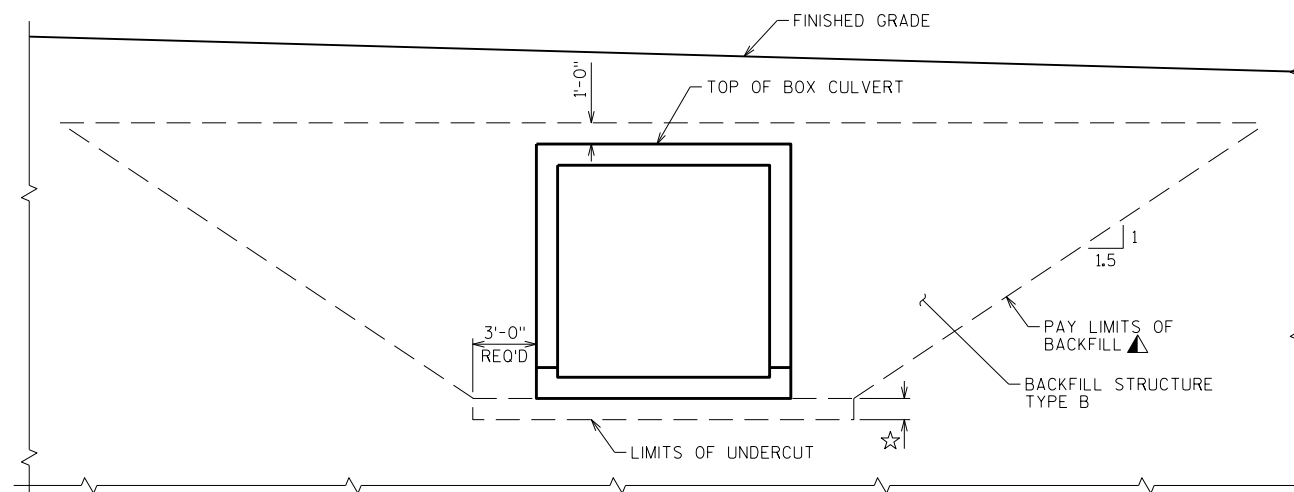
▲ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.



BAR SERIES TABLE

BAR MARK	NO. REQ'D	LENGTH
U403	1 SERIES OF 17	15'-10" TO 24'-8"
D405	1 SERIES OF 4	4'-1" TO 14'-9"
D406	1 SERIES OF 4	2'-9" TO 14'-6"
D410	2 SERIES OF 5	1'-9" TO 16'-5"
D512	1 SERIES OF 27	10'-8" TO 15'-0"
D513	1 SERIES OF 9	9'-11" TO 12'-5"
D414	1 SERIES OF 13	1'-5" TO 8'-7"
D418	2 SERIES OF 5	1'-11" TO 17'-10"
D520	1 SERIES OF 29	10'-8" TO 15'-0"
D521	1 SERIES OF 10	9'-11" TO 12'-6"
D422	1 SERIES OF 14	1'-5" TO 8'-7"
U403	1 SERIES OF 17	16'-9" TO 43'-10"
U405	1 SERIES OF 20	1'-8" TO 17'-3"
U406	1 SERIES OF 7	2'-0" TO 16'-0"
U410	2 SERIES OF 5	2'-11" TO 27'-11"
U512	1 SERIES OF 45	10'-8" TO 15'-0"
U513	1 SERIES OF 15	9'-11" TO 12'-7"
U414	1 SERIES OF 22	1'-5" TO 8'-7"
U418	2 SERIES OF 5	1'-10" TO 16'-10"
U520	1 SERIES OF 27	10'-8" TO 15'-0"
U521	1 SERIES OF 9	9'-11" TO 12'-6"
U422	1 SERIES OF 14	1'-5" TO 8'-7"

BUNDLE AND TAG EACH SERIES SEPERATELY



TYPICAL SECTION THRU BOX CULVERT

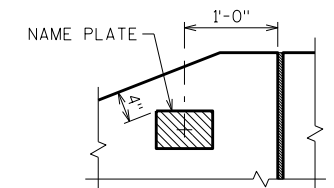
▲ BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

☆ UNDER CUT 2'-0". EXCAVATION FOR UNDER CUT TO BE INCLUDED IN EXCAVATION FOR STRUCTURES. PLACE "GEOTEXTILE TYPE C" AND BACKFILL WITH "BREAKER RUN".

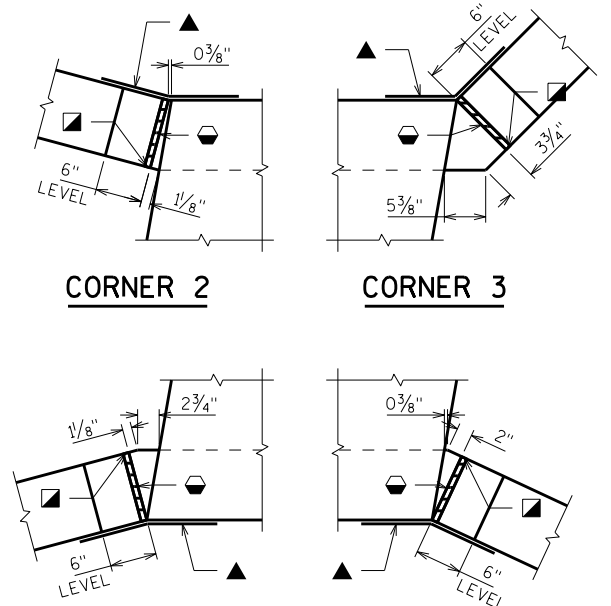
STATE PROJECT NUMBER

7080-01-72

- 3/4" FILLER, TYP. EXTEND FILLER FROM HORIZ. CONST. JT. TO TOP OF WING.
- 1" BEVEL, TYP.
- ▲ 18" RUBBERIZED MEMBRANE WATERPROOFING, EXTEND FROM HORIZ. CONST. JT. TO TOP OF WALL.



NAME PLATE LOCATION WING 4



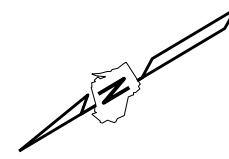
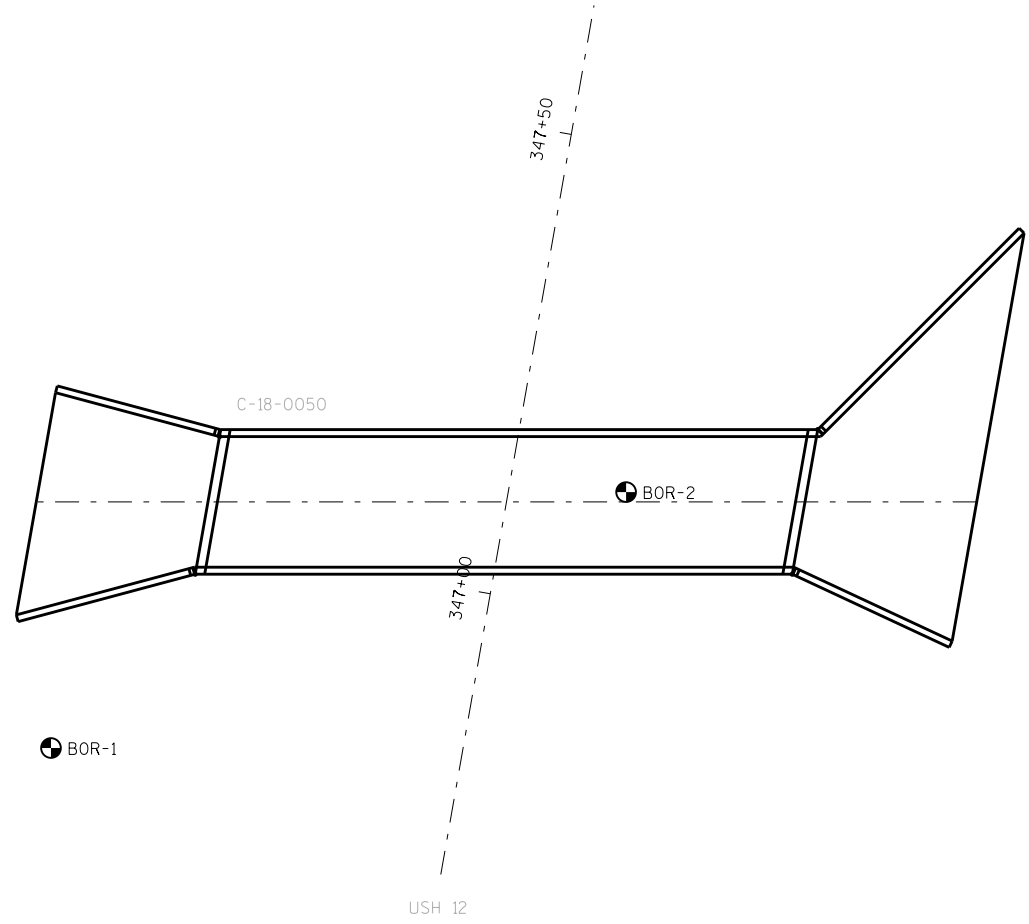
CORNER DETAILS

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-18-50			
DRAWN BY		AA	PLANS CK'D. MWB
APRON DETAILS			SHEET 4

SCALE = 1:00

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	4/16/2019	266661	382253
2	5/14/2019	266594	382245

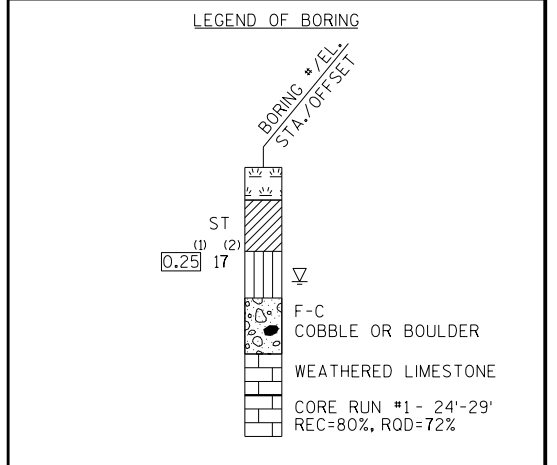
BORINGS COMPLETED BY: WISDOT
 REPORT COMPLETED BY: WISDOT
 ALL COORDINATES REFERENCED TO WCCS NAD 83(9D) EAU CLAIRE COUNTY
 COORDINATES COLLECTED USING NON-SURVEY GRADE EQUIPMENT



STATE PROJECT NUMBER
7080-01-72

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/META



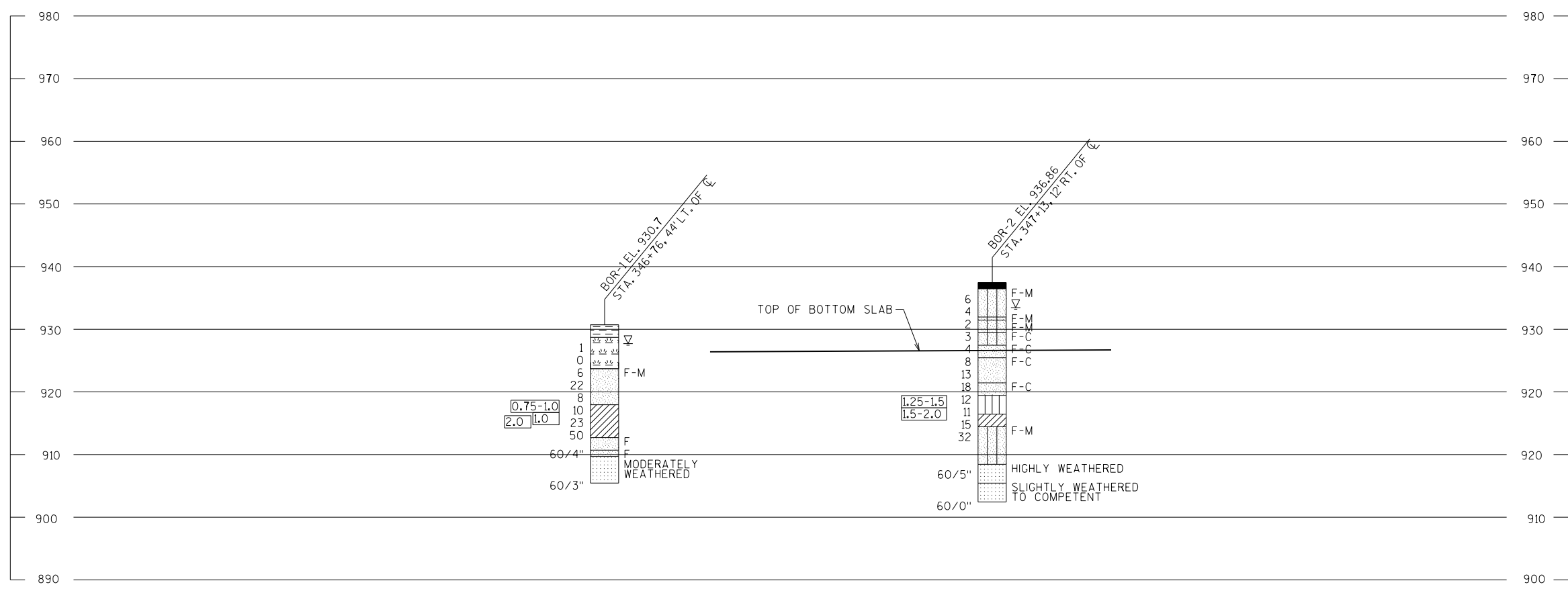
(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)
 (2) UNLESS OTHERWISE SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION
 ▽ AT TIME OF DRILLING
 ▽ END OF DRILLING
 ▽ AFTER DRILLING

ABBREVIATIONS
 F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.



8

8

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE C-18-50			
DRAWN BY TLP/AA		PLANS CKD. MWB	
SUBSURFACE EXPLORATION		SHEET 5	

SCALE = 10

NOTES:

- 1 - CUT CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
- 2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL THIS DOES NOT SHOW UP IN CROSS SECTIONS
- 3 - FILL DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
- 4 - EXPANDED MARSH BACKFILL WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)
- 5 - EXPANDED EBS WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)
- 6 - REDUCED MARSH IN FILL REDUCED MARSH EXCAVATION THAT CAN BE USED IN FILL
- 7 - REDUCED EBS IN FILL REDUCED EBS EXCAVATION THAT CAN BE USED IN FILL
- 8 - MASS ORDINATE IF MARSH OR EBS TO BE BACKFILLED WITH COMMON OR BORROW:
 $[(\text{CUT} - \text{SALVAGED PAVT} - \text{EXPANDED MARSH EXC} - \text{EXPANDED EBS}) - ((\text{FILL} - \text{REDUCED MARSH IN FILL} - \text{REDUCED EBS IN FILL} - \text{EXPANDED ROCK}) * \text{FILL FACTOR})]$
- 8 - MASS ORDINATE IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR:
 $[\text{CUT} - \text{SALVAGED PAVT} - ((\text{FILL} - \text{REDUCED MARSH IN FILL} - \text{REDUCED EBS IN FILL} - \text{EXPANDED ROCK}) * \text{FILL FACTOR})]$
- 8 - MASS ORDINATE IF MARSH AND EBS TO BE BACKFILLED WITH COMMON OR BORROW:
 $[(\text{CUT} - \text{SALVAGED PAVT} - \text{EXPANDED MARSH EXC} - \text{EXPANDED EBS}) - ((\text{FILL} - \text{EXPANDED ROCK}) * \text{FILL FACTOR})]$
- 8 - MASS ORDINATE IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: $[\text{CUT} - \text{SALVAGED PAVT} - ((\text{FILL} - \text{EXPANDED ROCK}) * \text{FILL FACTOR})]$

Division 1 - TempBp-SixMile										
STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EXPANDED		MASS ORDINATE
								1.00	1.25	
NOTE 1	NOTE 2	NOTE 3	NOTE 1	NOTE 8						
00+50'BP1	0.00	0.00	0.00	64.93	0	0	0	0	0	0
01+00'BP1	50.00	0.00	0.00	738.59	0	0	744	0	930	-930
01+50'BP1	50.00	0.00	0.00	356.10	0	0	1,014	0	2,197	-2,197
02+00'BP1	50.00	0.91	0.00	64.44	1	0	389	1	2,684	-2,683
02+50'BP1	50.00	15.15	0.00	0.19	15	0	60	16	2,759	-2,743
03+00'BP1	50.00	41.78	0.00	0.00	53	0	0	68	2,759	-2,690
03+50'BP1	50.00	34.52	0.00	4.07	71	0	4	139	2,763	-2,624
04+00'BP1	50.00	12.89	0.00	15.12	44	0	18	183	2,786	-2,603
04+50'BP1	50.00	12.79	0.00	24.55	24	0	37	207	2,832	-2,625
05+00'BP1	50.00	10.08	0.00	59.08	21	0	77	228	2,928	-2,700
05+50'BP1	50.00	9.03	0.00	72.51	18	0	122	246	3,081	-2,835
06+00'BP1	50.00	10.32	0.00	16.49	18	0	82	264	3,184	-2,920
06+50'BP1	50.00	23.12	0.00	0.18	31	0	15	294	3,203	-2,909
07+00'BP1	50.00	16.92	0.00	0.18	37	0	0	332	3,203	-2,872
07+50'BP1	50.00	11.25	0.00	0.13	26	0	0	358	3,204	-2,846
08+00'BP1	50.00	9.75	0.00	0.12	19	0	0	377	3,204	-2,827
08+50'BP1	50.00	9.47	0.00	0.00	18	0	0	395	3,204	-2,809
09+00'BP1	50.00	10.80	0.00	0.00	19	0	0	414	3,204	-2,791
09+50'BP1	50.00	10.43	0.00	0.00	20	0	0	433	3,204	-2,771
10+00'BP1	50.00	10.90	0.00	0.00	20	0	0	453	3,204	-2,751
10+28.01'BP1	28.01	10.95	0.00	0.00	11	0	0	464	3,204	-2,740
CUMULATIVE TOTALS					464	0	2,563			

9

9

Division 1 - TempBp-NineMile										
STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EXPANDED		
								1.00	1.25	MASS ORDINATE
NOTE 1	NOTE 2	NOTE 3	NOTE 1	NOTE 8						
00+00'BP2	0.00	10.25	0.00	0.00	0	0	0	0	0	0
00+50'BP2	50.00	8.96	0.00	0.00	18	0	0	18	0	18
01+00'BP2	50.00	8.02	0.00	0.00	16	0	0	34	0	34
01+50'BP2	50.00	7.31	0.00	3.40	14	0	3	48	4	44
02+00'BP2	50.00	7.81	0.00	5.95	14	0	9	62	15	47
02+50'BP2	50.00	9.16	0.00	3.60	16	0	9	77	26	52
03+00'BP2	50.00	8.68	0.00	0.00	17	0	3	94	30	64
03+50'BP2	50.00	11.27	0.00	0.00	18	0	0	112	30	82
04+00'BP2	50.00	12.08	0.00	0.00	22	0	0	134	30	104
04+50'BP2	50.00	9.93	0.00	12.04	20	0	11	154	44	111
05+00'BP2	50.00	14.05	0.00	22.87	22	0	32	177	84	92
05+50'BP2	50.00	23.86	0.00	23.94	35	0	43	212	138	73
06+00'BP2	50.00	28.96	0.00	30.25	49	0	50	261	201	59
06+50'BP2	50.00	27.27	0.00	37.26	52	0	63	313	279	33
07+00'BP2	50.00	29.34	0.00	51.99	52	0	83	365	383	-17
07+50'BP2	50.00	27.34	0.00	72.01	52	0	115	418	526	-109
08+00'BP2	50.00	23.93	0.00	97.79	47	0	157	465	723	-258
08+50'BP2	50.00	18.03	0.00	144.25	39	0	224	504	1,003	-499
09+00'BP2	50.00	17.26	0.00	182.90	33	0	303	537	1,381	-845
09+50'BP2	50.00	25.23	0.00	185.99	39	0	342	576	1,808	-1,232
10+00'BP2	50.00	40.76	0.00	178.66	61	0	338	637	2,230	-1,593
10+50'BP2	50.00	39.75	0.00	242.50	75	0	390	712	2,718	-2,006
11+00'BP2	50.00	15.00	0.00	329.22	51	0	529	762	3,380	-2,617
11+50'BP2	50.00	0.07	0.00	322.56	14	0	603	776	4,134	-3,358
12+00'BP2	50.00	0.35	0.00	289.85	0	0	567	777	4,843	-4,066
12+50'BP2	50.00	5.34	0.00	228.62	5	0	480	782	5,443	-4,661
13+00'BP2	50.00	21.98	0.00	186.58	25	0	384	807	5,923	-5,116
13+50'BP2	50.00	15.29	0.00	151.50	35	0	313	842	6,315	-5,473
14+00'BP2	50.00	14.17	0.00	120.65	27	0	252	869	6,630	-5,761
14+50'BP2	50.00	13.10	0.00	95.11	25	0	200	894	6,879	-5,985
15+00'BP2	50.00	16.30	0.00	68.84	27	0	152	921	7,069	-6,148
15+50'BP2	50.00	14.23	0.00	52.60	28	0	112	950	7,210	-6,260
16+00'BP2	50.00	13.52	0.00	43.04	26	0	89	975	7,320	-6,345
16+50'BP2	50.00	9.23	0.00	25.95	21	0	64	997	7,400	-6,404
17+00'BP2	50.00	9.37	0.00	18.93	17	0	42	1,014	7,452	-6,438
17+50'BP2	50.00	9.46	0.00	9.93	17	0	27	1,031	7,486	-6,454
18+00'BP2	50.00	9.50	0.00	2.45	18	0	11	1,049	7,500	-6,451
18+50'BP2	50.00	9.79	0.00	0.00	18	0	2	1,067	7,503	-6,436
19+00'BP2	50.00	10.25	0.00	0.00	19	0	0	1,085	7,503	-6,418
19+50'BP2	50.00	10.22	0.00	0.00	19	0	0	1,104	7,503	-6,399
20+00'BP2	50.00	9.51	0.00	0.00	18	0	0	1,122	7,503	-6,380
20+50'BP2	50.00	9.92	0.00	0.00	18	0	0	1,140	7,503	-6,362
21+00'BP2	50.00	8.33	0.00	0.45	17	0	0	1,157	7,503	-6,346
21+32.29'BP2	32.29	8.33	0.00	0.45	10	0	1	1,167	7,504	-6,337
CUMULATIVE TOTALS					1,167	0	6,003			

Division 1 - USH12 Exist											
STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)			
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EXPANDED			
								NOTE 1	NOTE 2	NOTE 3	CUT 1.00
237+10.85	0.00	142.90	0.00	17.65	0	0	0	0	0	0	
237+29.43	18.58	161.91	0.00	126.44	105	0	50	105	62	43	
237+95.83	0.00	789.07	0.00	182.35	0	0	0	105	62	43	
238+00.00	4.17	790.73	0.00	153.03	122	0	26	227	94	132	
238+13.81	13.81	803.76	0.00	64.69	408	0	56	635	164	471	
238+25.83	12.02	606.02	0.00	13.20	314	0	17	948	186	763	
238+50.00	24.17	247.83	0.00	23.43	382	0	16	1,331	206	1,125	
238+60.84	10.84	237.54	0.00	8.56	97	0	6	1,428	214	1,214	
239+00.00	39.16	132.49	0.00	0.05	268	0	6	1,696	222	1,474	
239+50.00	50.00	88.74	0.00	4.12	205	0	4	1,901	227	1,674	
240+00.00	50.00	88.01	0.00	6.89	164	0	10	2,065	240	1,825	
240+50.00	50.00	85.14	0.00	4.76	160	0	11	2,225	253	1,972	
241+00.00	50.00	82.24	0.00	0.48	155	0	5	2,380	259	2,121	
241+50.00	50.00	83.78	0.00	0.00	154	0	0	2,534	260	2,274	
242+00.00	50.00	99.02	0.00	0.00	169	0	0	2,703	260	2,444	
242+50.00	50.00	65.04	0.00	0.00	152	0	0	2,855	260	2,595	
243+00.00	50.00	36.71	0.00	0.62	94	0	1	2,949	260	2,689	
243+50.00	50.00	27.05	0.00	4.86	59	0	5	3,008	267	2,742	
244+00.00	50.00	22.16	0.00	0.42	46	0	5	3,054	273	2,781	
244+50.00	50.00	17.02	0.00	0.00	36	0	0	3,090	273	2,817	
245+00.00	50.00	11.50	0.00	2.18	26	0	2	3,117	276	2,841	
245+50.00	50.00	13.82	0.00	0.00	23	0	2	3,140	278	2,862	
246+00.00	50.00	11.99	0.00	0.00	24	0	0	3,164	278	2,886	
246+50.00	50.00	12.07	0.00	2.18	22	0	2	3,186	281	2,905	
247+00.00	50.00	14.29	0.00	0.00	24	0	2	3,211	283	2,927	
247+03.74	3.74	14.32	0.00	0.00	2	0	0	3,213	283	2,929	
336+33.42	0.00	10.36	0.00	8.08	0	0	0	3,213	283	2,929	
337+00.00	66.58	10.32	0.00	20.59	25	0	35	3,238	328	2,911	
337+50.00	50.00	10.11	0.00	13.68	19	0	32	3,257	367	2,890	
338+00.00	50.00	10.10	0.00	26.70	19	0	37	3,276	414	2,862	
338+50.00	50.00	10.81	0.00	8.91	19	0	33	3,295	455	2,840	
339+00.00	50.00	11.71	0.00	4.48	21	0	12	3,316	471	2,845	
339+50.00	50.00	23.13	0.00	0.00	32	0	4	3,348	476	2,872	
CUMULATIVE SUBTOTALS					3,348	0	381				

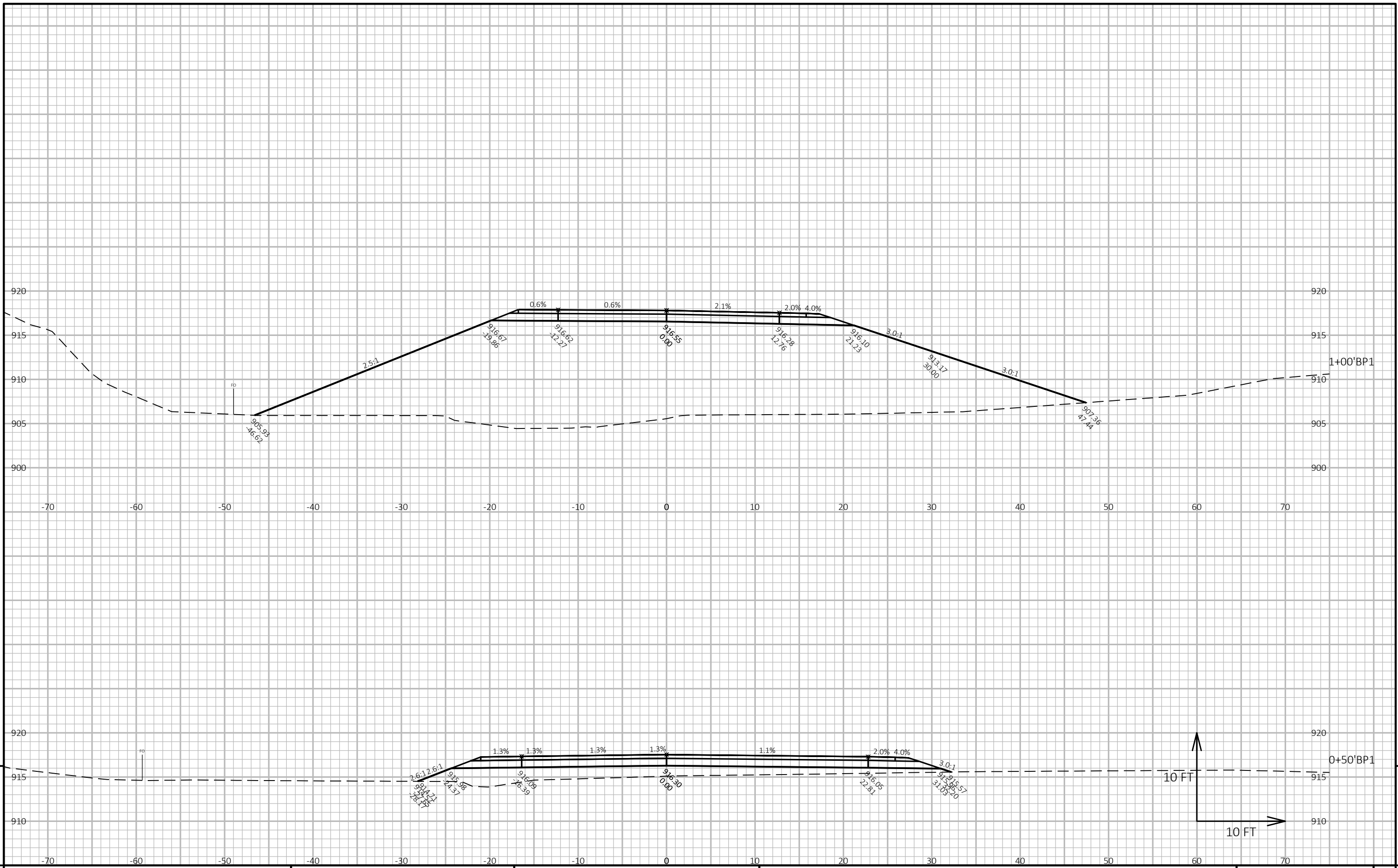
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Division 1 - USH12 Exist										
STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EXPANDED		
								1.00	1.25	MASS ORDINATE
NOTE 1	NOTE 2	NOTE 3	NOTE 1	NOTE 8						
340+00.00	50.00	26.53	0.00	0.00	46	0	0	3,394	476	2,918
340+50.00	50.00	25.98	0.00	0.05	49	0	0	3,443	476	2,967
341+00.00	50.00	43.05	0.00	0.00	64	0	0	3,507	476	3,031
341+50.00	50.00	59.44	0.00	3.67	95	0	3	3,602	480	3,121
342+00.00	50.00	66.34	0.00	8.65	116	0	11	3,718	494	3,224
342+50.00	50.00	77.59	0.00	8.35	133	0	16	3,851	514	3,337
343+00.00	50.00	96.82	0.00	9.96	161	0	17	4,013	535	3,477
343+50.00	50.00	126.16	0.00	15.23	206	0	23	4,219	565	3,655
344+00.00	50.00	153.46	0.00	14.12	259	0	27	4,478	599	3,880
344+50.00	50.00	189.78	0.00	11.87	318	0	24	4,796	629	4,167
345+00.00	50.00	240.55	0.00	8.68	398	0	19	5,195	652	4,542
345+50.00	50.00	271.74	0.00	16.03	474	0	23	5,669	681	4,988
346+00.00	50.00	336.26	0.00	25.98	563	0	39	6,232	730	5,502
346+50.00	50.00	339.21	0.00	39.31	625	0	60	6,857	805	6,052
346+88.27	38.27	341.89	0.00	90.45	483	0	92	7,340	920	6,420
347+48.43	0.00	412.14	0.00	23.28	0	0	0	7,340	920	6,420
347+50.00	1.57	412.61	0.00	22.81	24	0	1	7,364	922	6,442
348+00.00	50.00	406.22	0.00	6.77	758	0	27	8,122	956	7,166
348+50.00	50.00	358.23	0.00	0.03	708	0	6	8,830	964	7,866
349+00.00	50.00	270.20	0.00	0.00	582	0	0	9,412	964	8,448
349+50.00	50.00	217.59	0.00	16.91	452	0	16	9,863	984	8,880
350+00.00	50.00	187.04	0.00	6.25	375	0	21	10,238	1,010	9,228
350+50.00	50.00	151.27	0.00	9.28	313	0	14	10,551	1,028	9,523
351+00.00	50.00	124.15	0.00	5.86	255	0	14	10,806	1,046	9,760
351+50.00	50.00	79.71	0.00	0.22	189	0	6	10,995	1,053	9,942
352+00.00	50.00	73.53	0.00	6.48	142	0	6	11,137	1,061	10,076
352+50.00	50.00	58.50	0.00	0.05	122	0	6	11,259	1,068	10,191
353+00.00	50.00	34.03	0.00	0.49	86	0	0	11,345	1,069	10,276
353+50.00	50.00	26.17	0.00	0.00	56	0	0	11,401	1,069	10,331
354+00.00	50.00	17.85	0.00	0.00	41	0	0	11,441	1,069	10,372
354+50.00	50.00	12.99	0.00	1.56	29	0	1	11,470	1,071	10,399
355+00.00	50.00	10.98	0.00	1.17	22	0	3	11,492	1,074	10,418
355+50.00	50.00	10.87	0.00	0.40	20	0	1	11,512	1,076	10,436
356+00.00	50.00	10.59	0.00	2.77	20	0	3	11,532	1,080	10,452
356+50.00	50.00	10.45	0.00	3.64	19	0	6	11,552	1,087	10,464
357+00.00	50.00	10.08	0.00	3.23	19	0	6	11,571	1,095	10,476
357+50.00	50.00	10.46	0.00	5.48	19	0	8	11,590	1,105	10,484
357+62.13	12.13	10.45	0.00	5.48	5	0	2	11,594	1,108	10,486
CUMULATIVE SUBTOTALS					8,246	0	506			
CUMULATIVE TOTALS					11,594	0	887			

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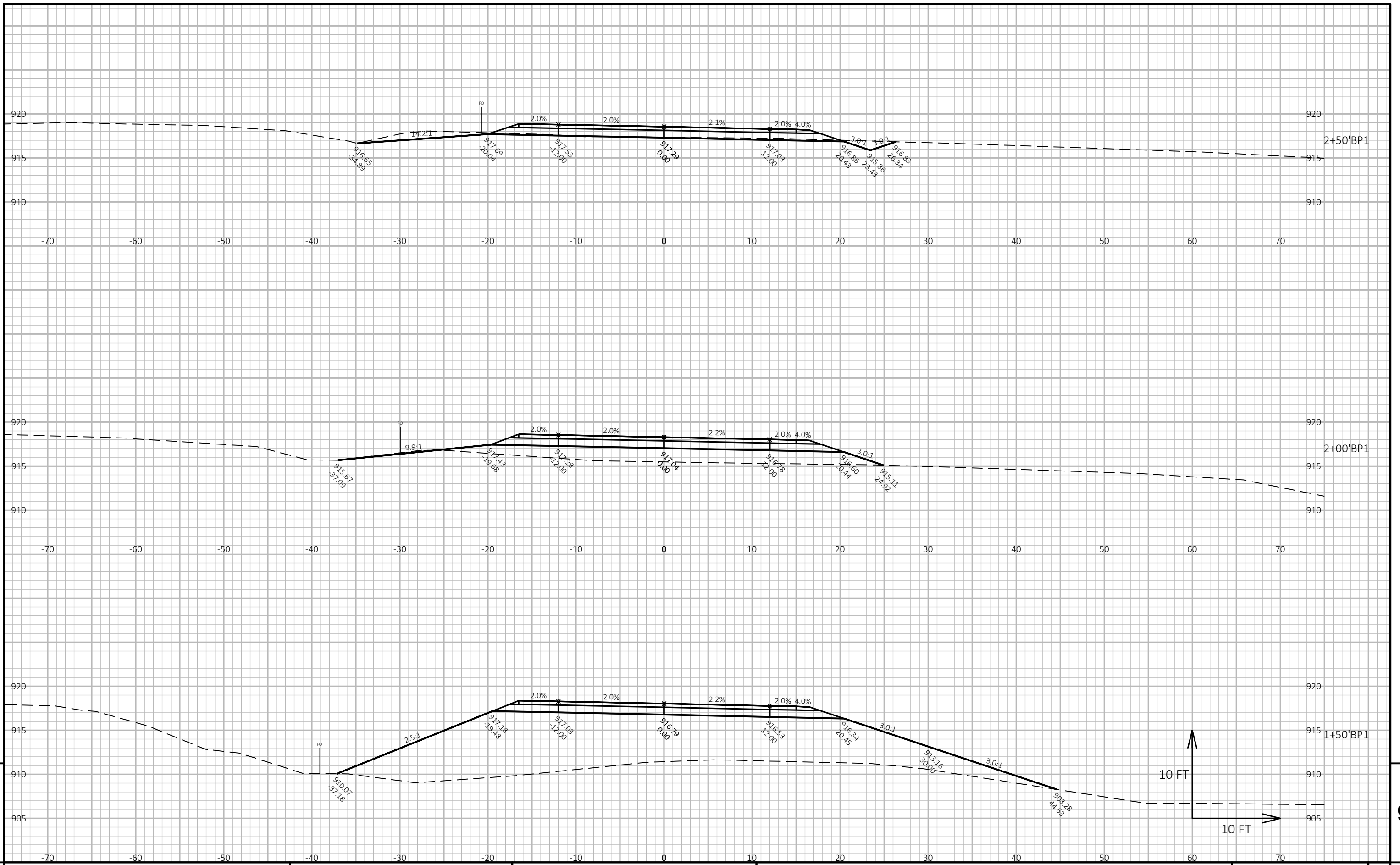


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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS SHEET E

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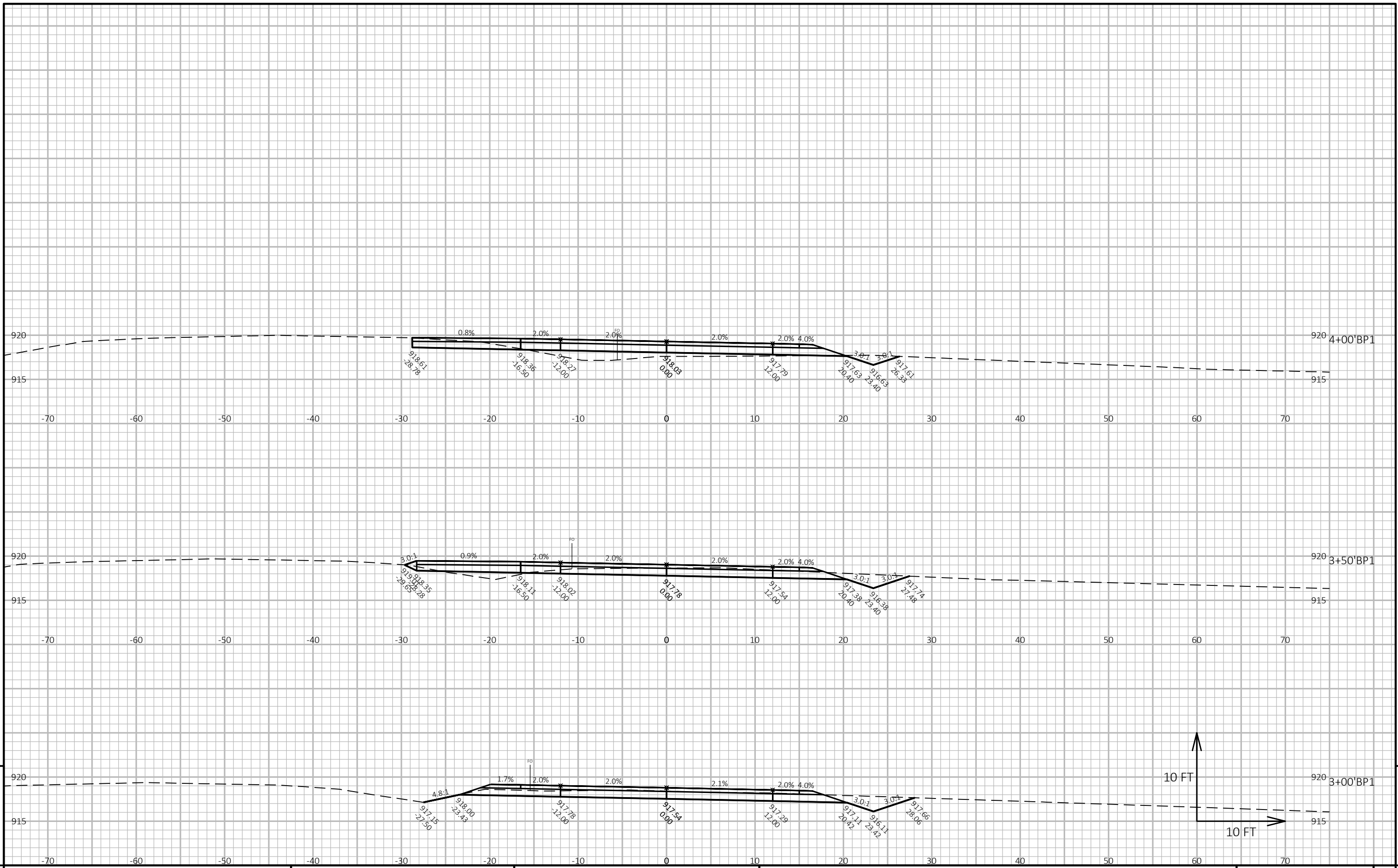


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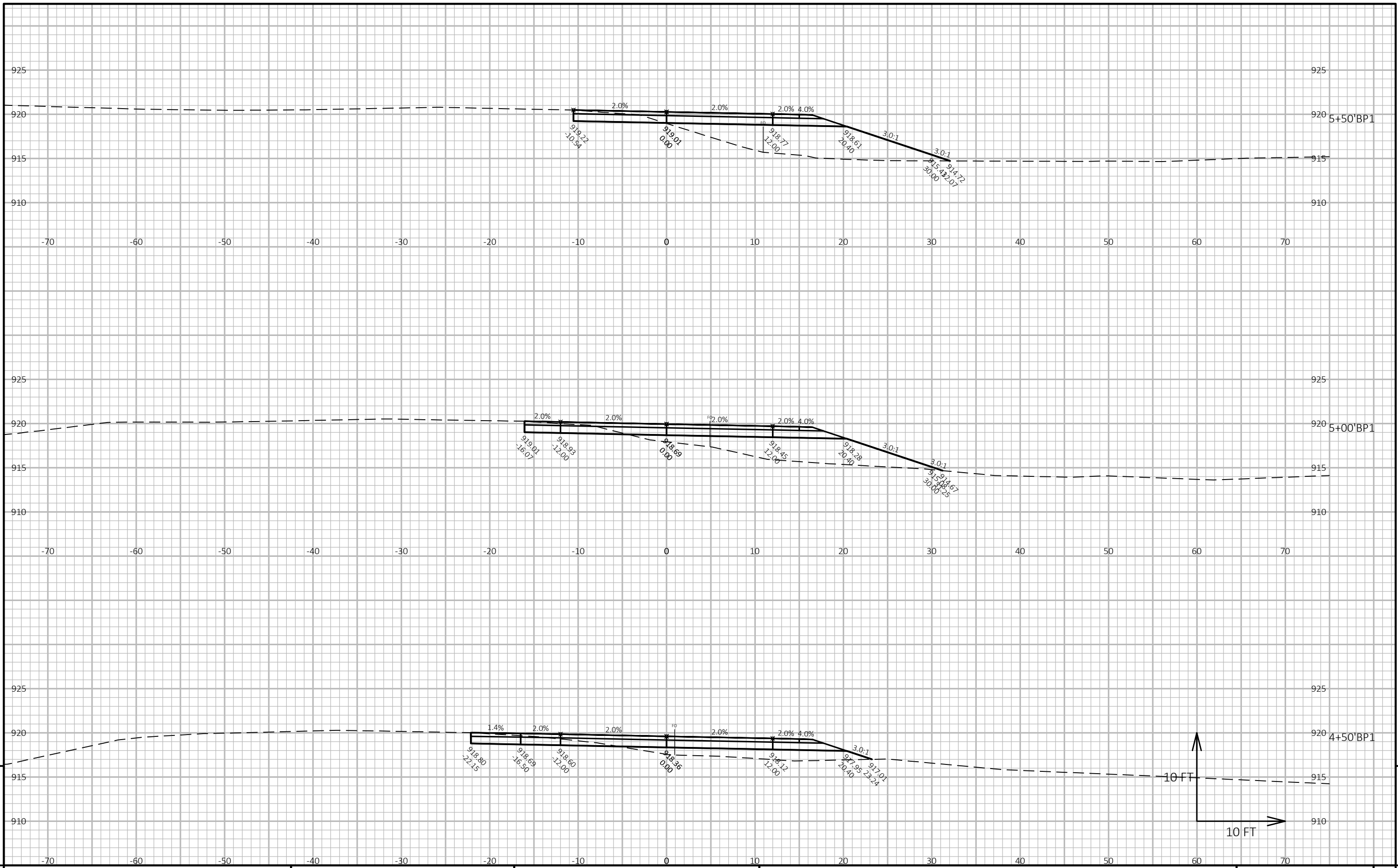
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS SHEET E

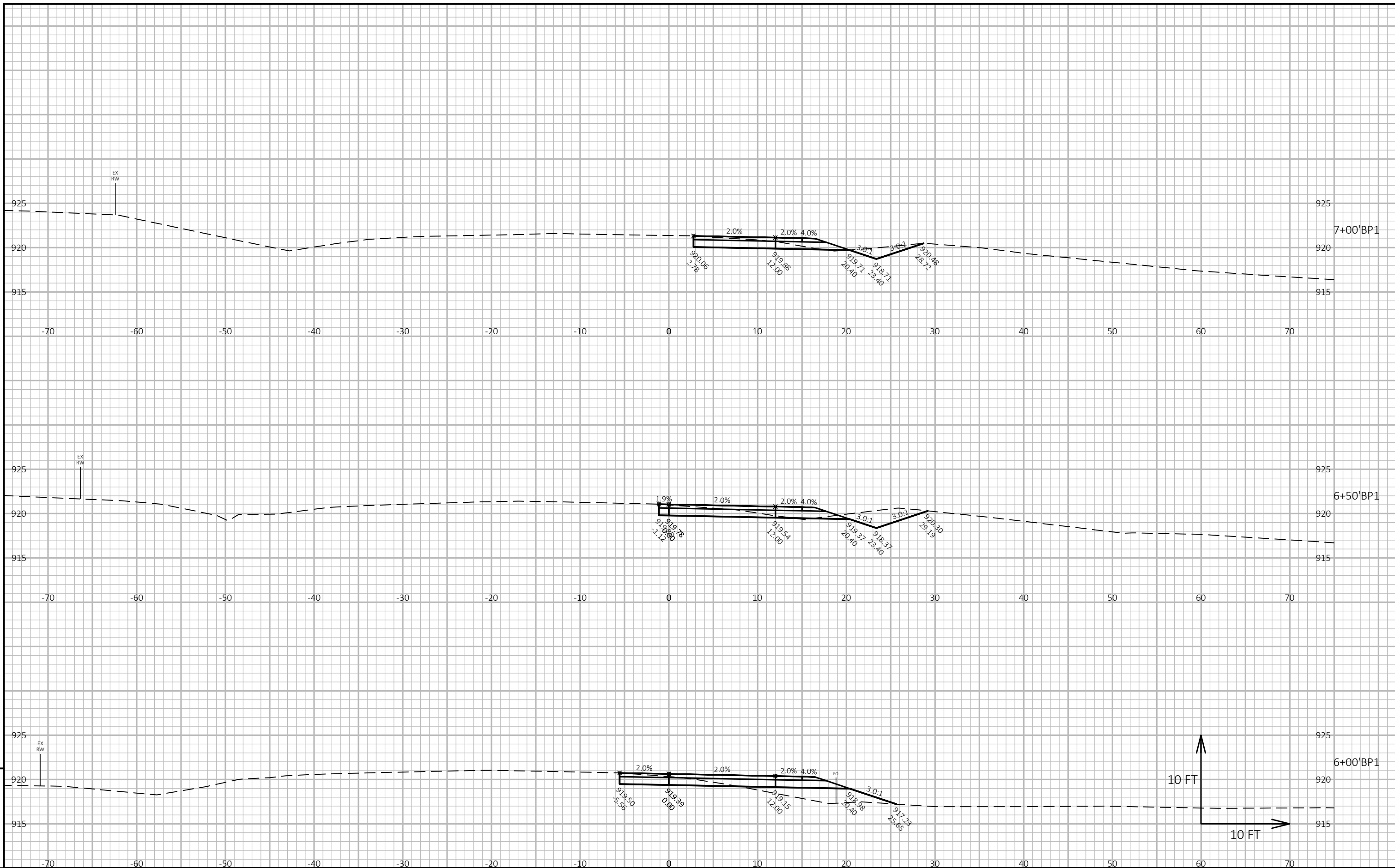
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS SHEET E



PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS SHEET E



PROJECT NO: 7080-01-72

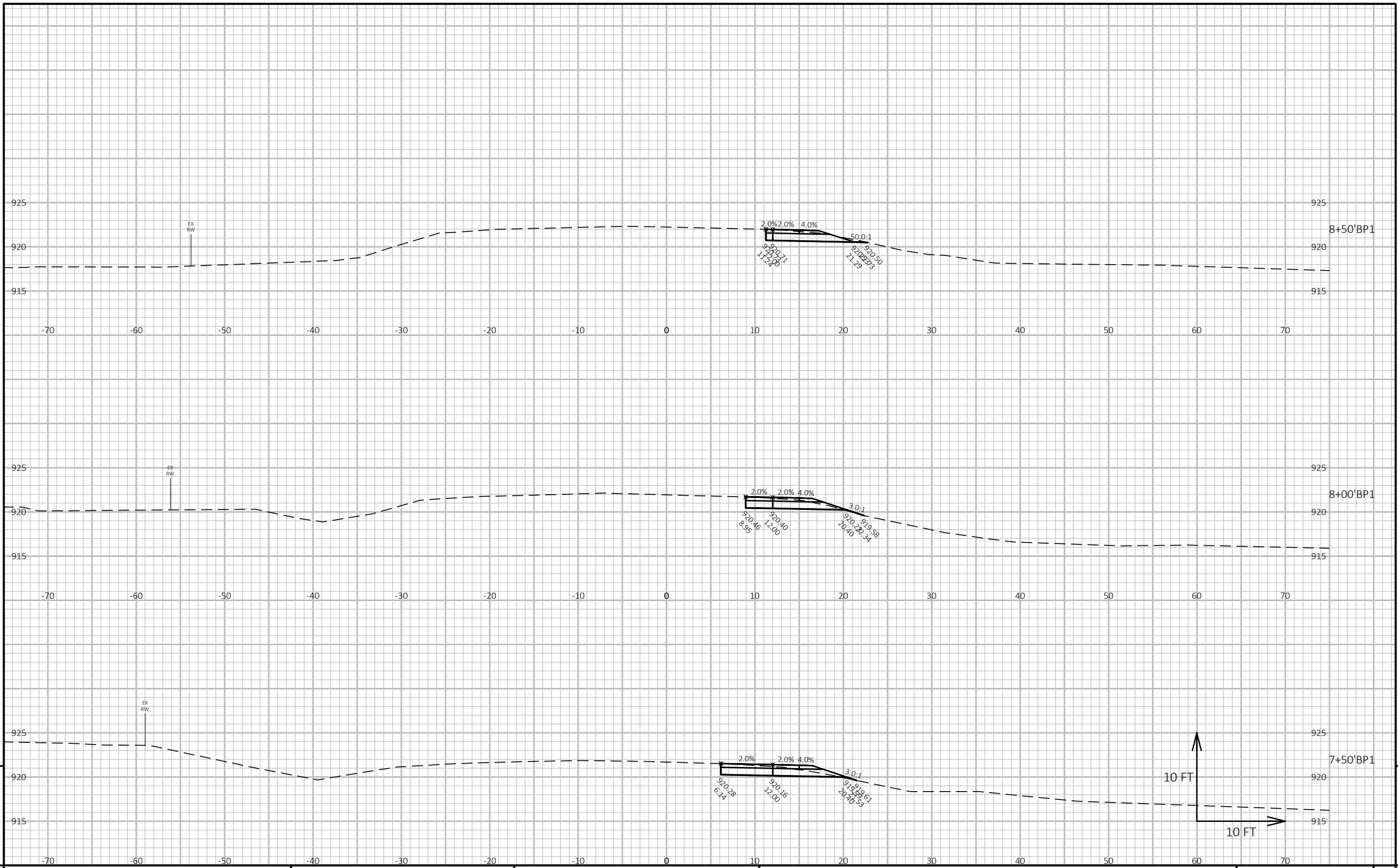
HWY: USH 12

COUNTY: EAU CLAIRE

CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS

SHEET

E



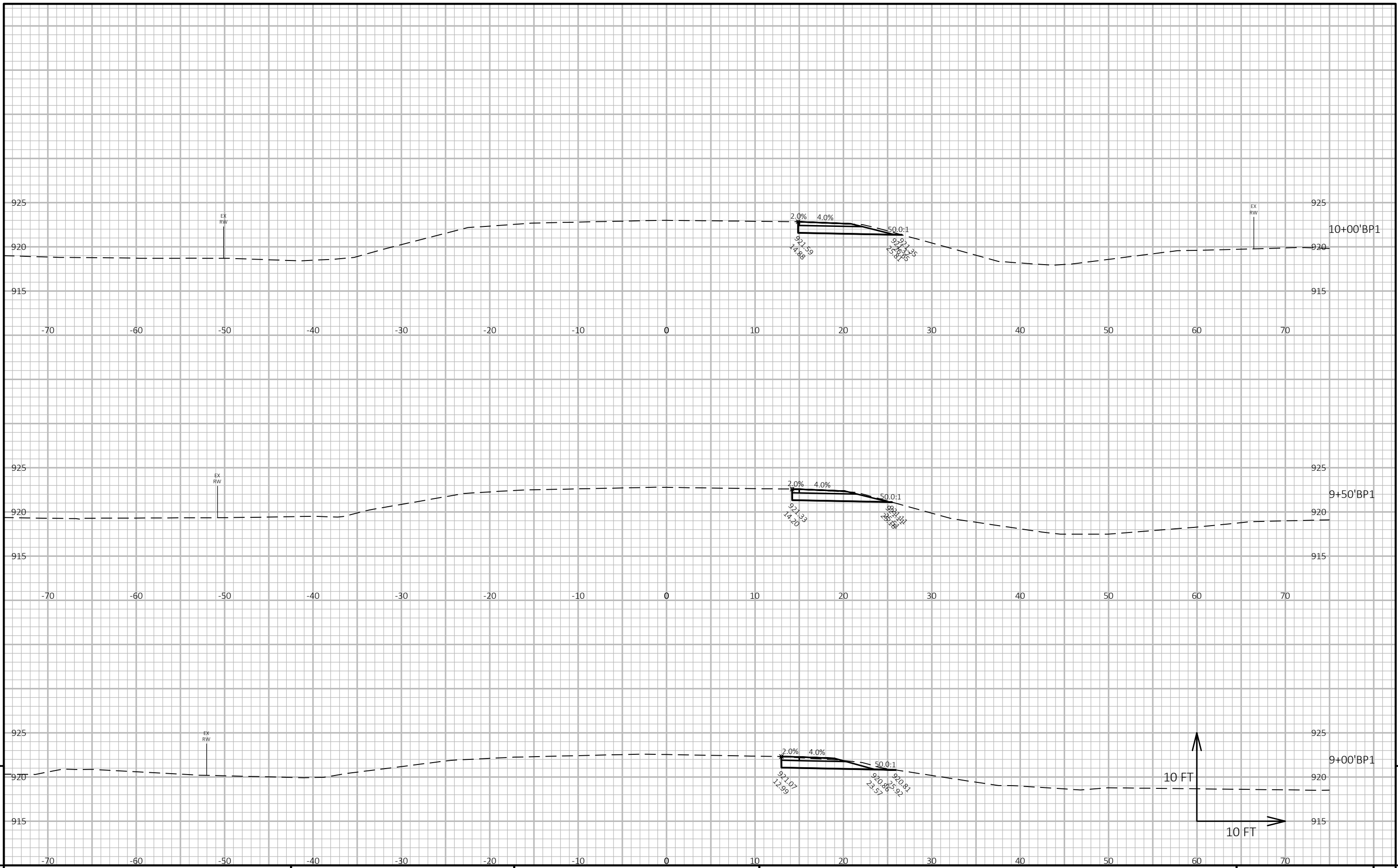
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS SHEET E

FILE NAME : \\EAURTOPFLPPI01\N3PUBLIC\PD\3\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_BP.DWG PLOT DATE : 11/1/2019 9:55 AM PLOT BY : PAYNE, MATTHEW A PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090206



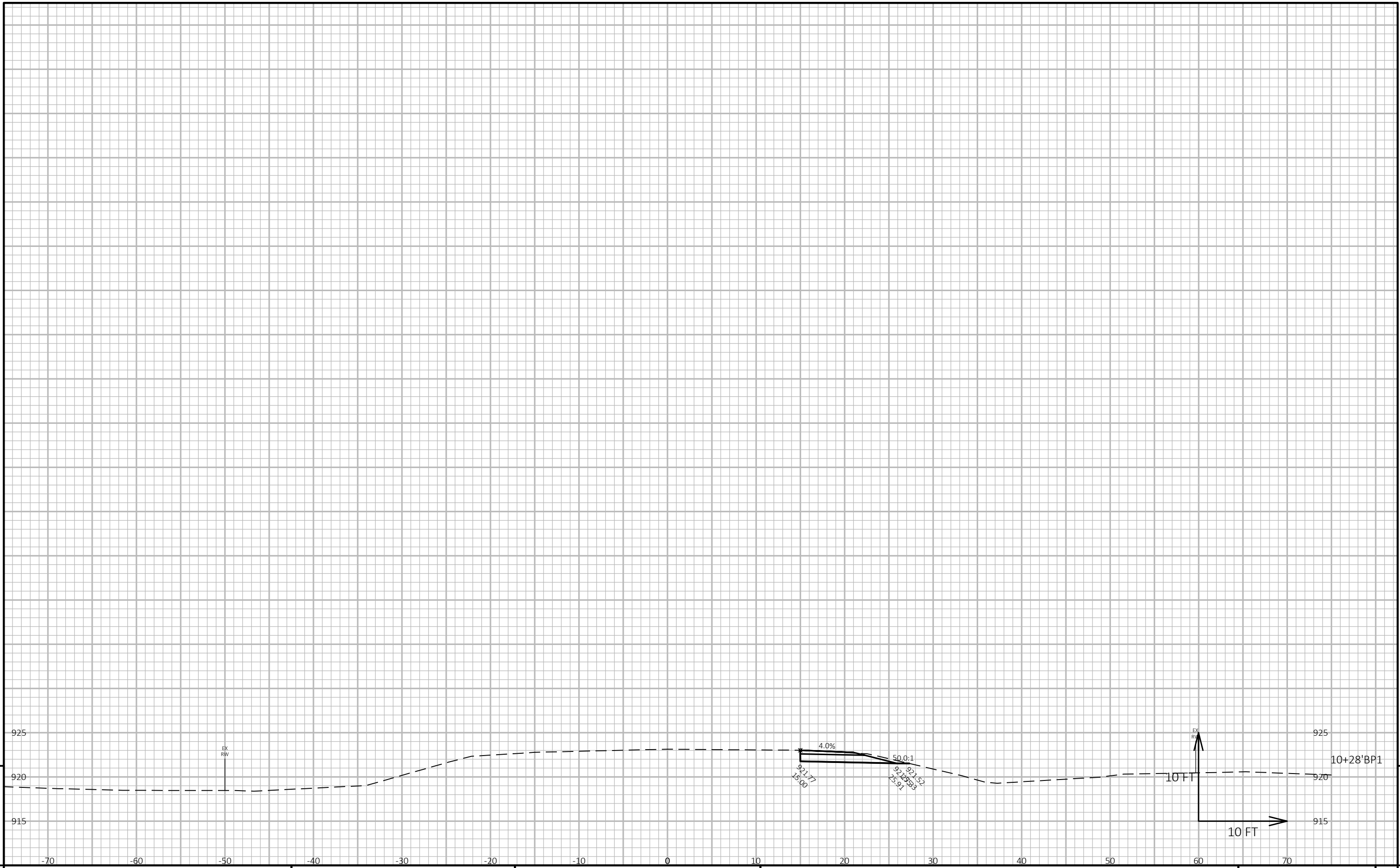
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS SHEET E

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LAYOUT NAME - 090207



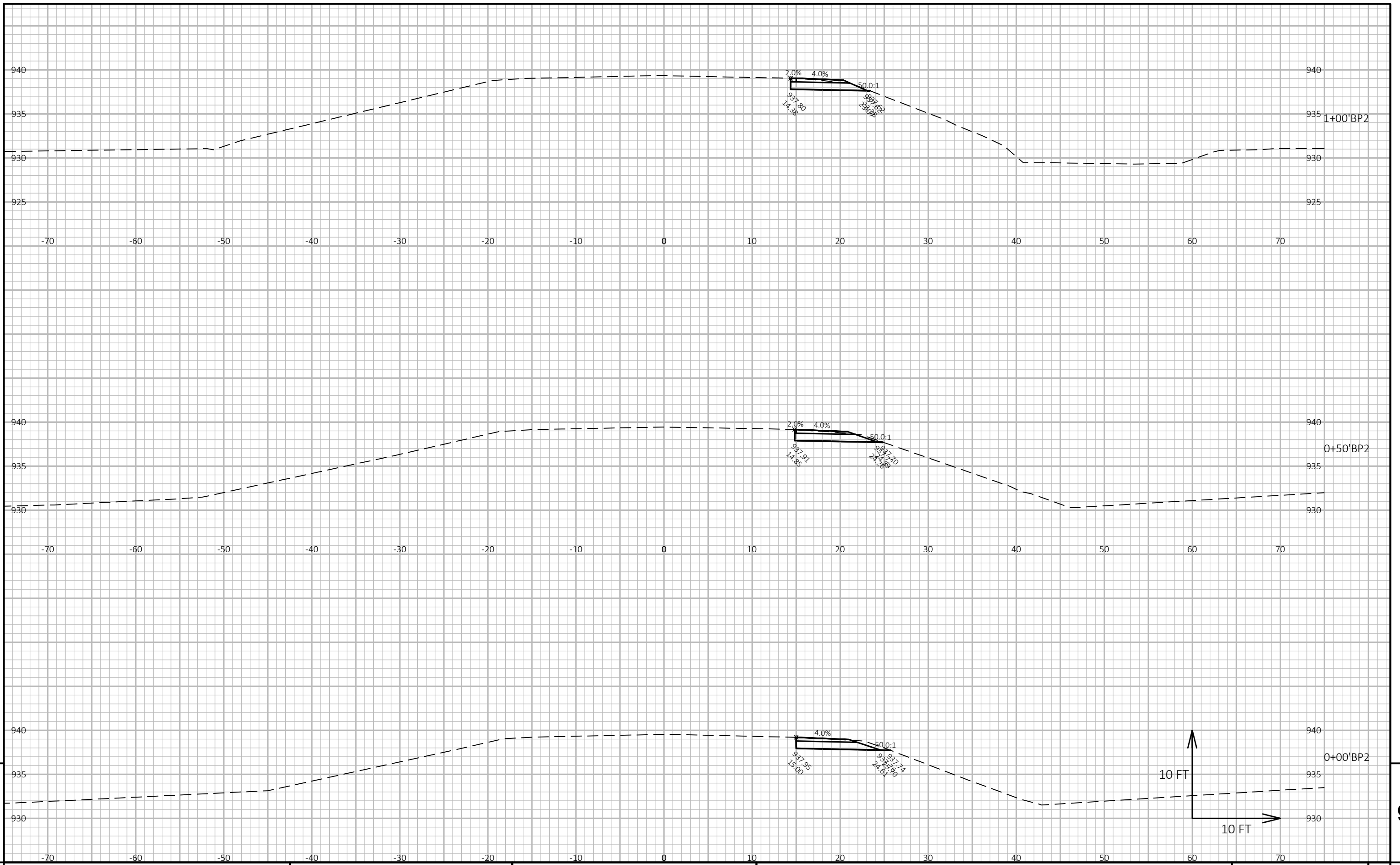
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PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	CROSS SECTIONS: SIX MILE CREEK TEMPORARY BYPASS	SHEET	E
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FILE NAME : \\EAURTOPFLPPI01\N3PUBLIC\PD\3\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_BP.DWG PLOT DATE : 11/1/2019 9:56 AM PLOT BY : PAYNE, MATTHEW A PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090208



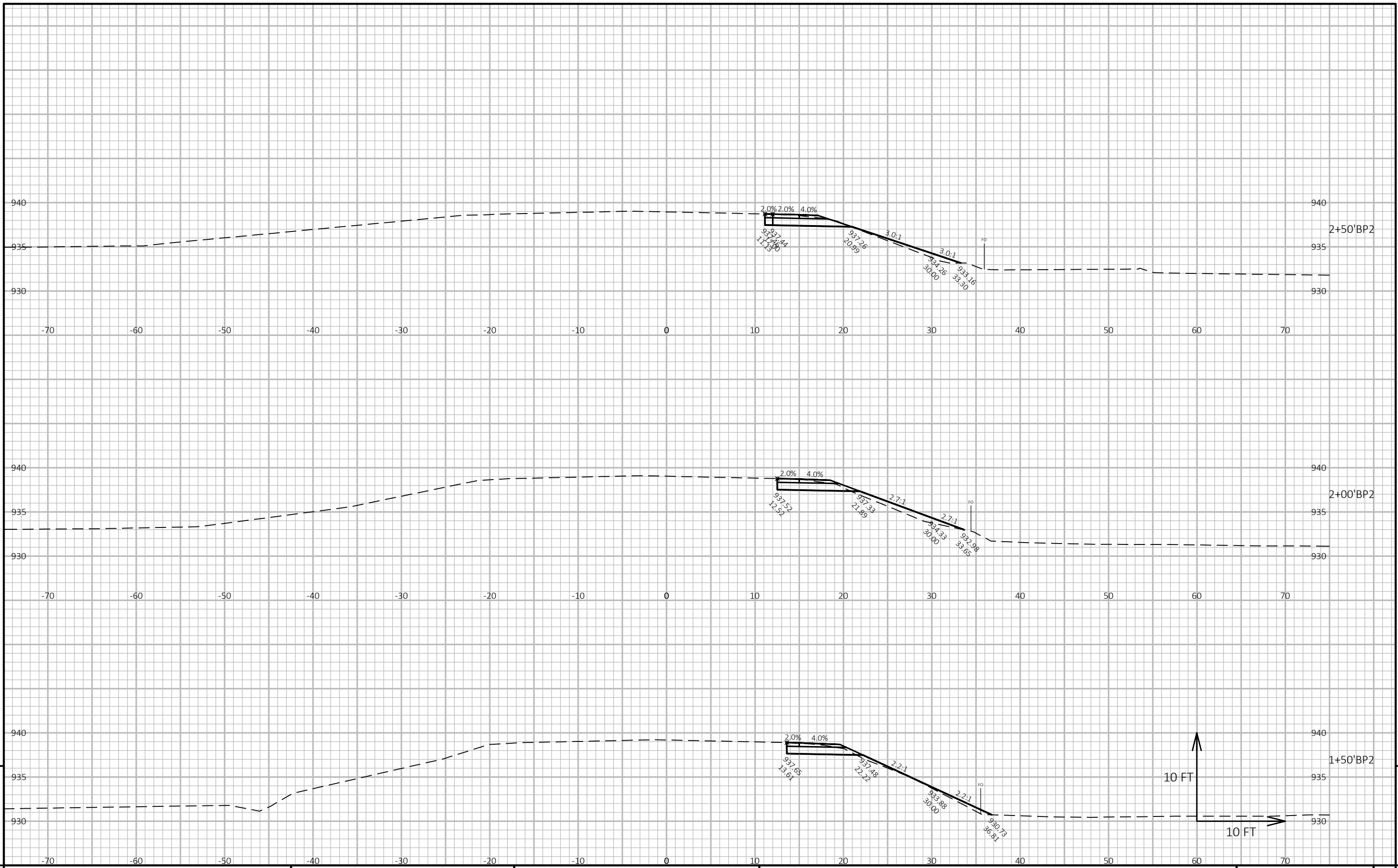
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E

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LAYOUT NAME - 090209



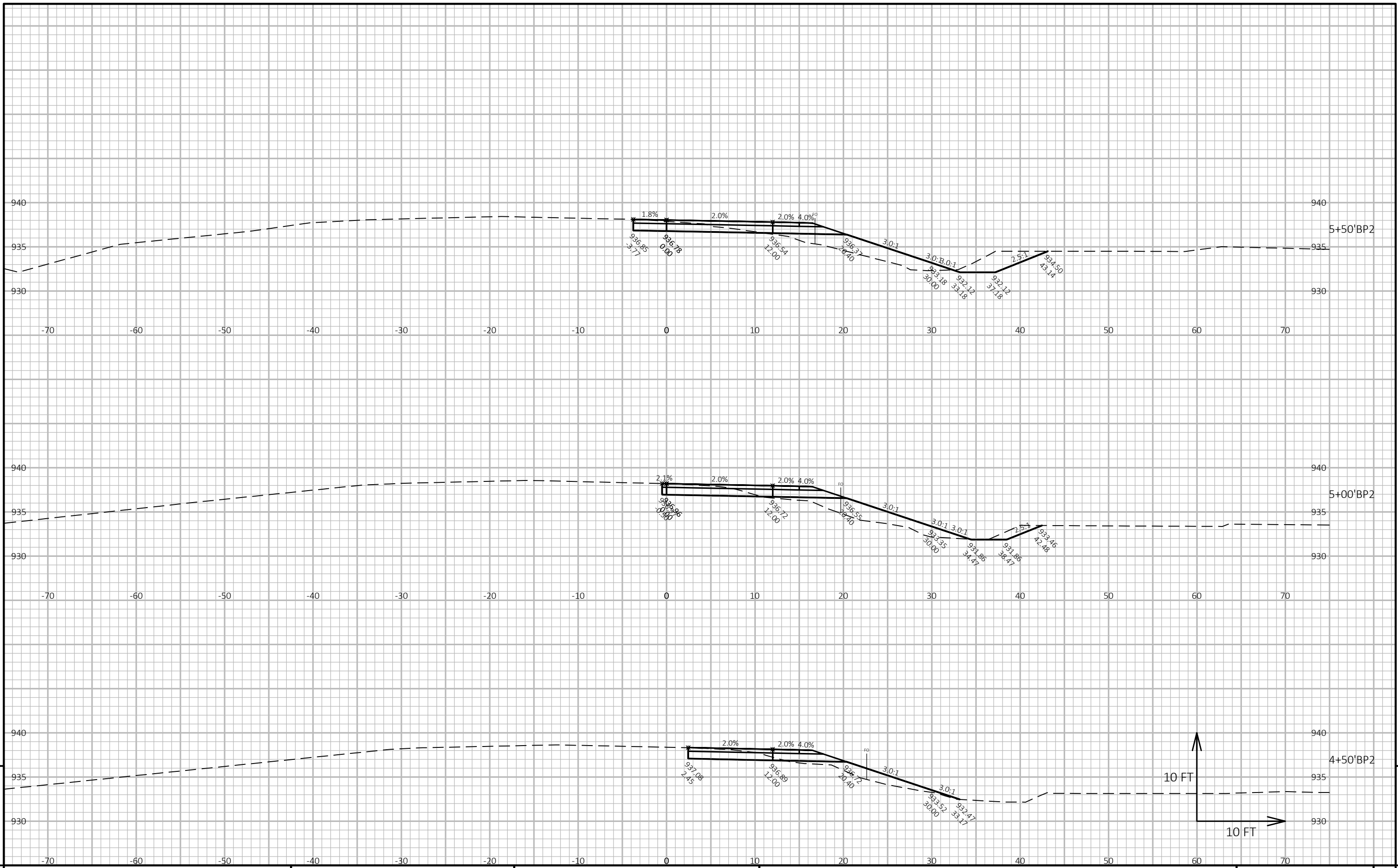
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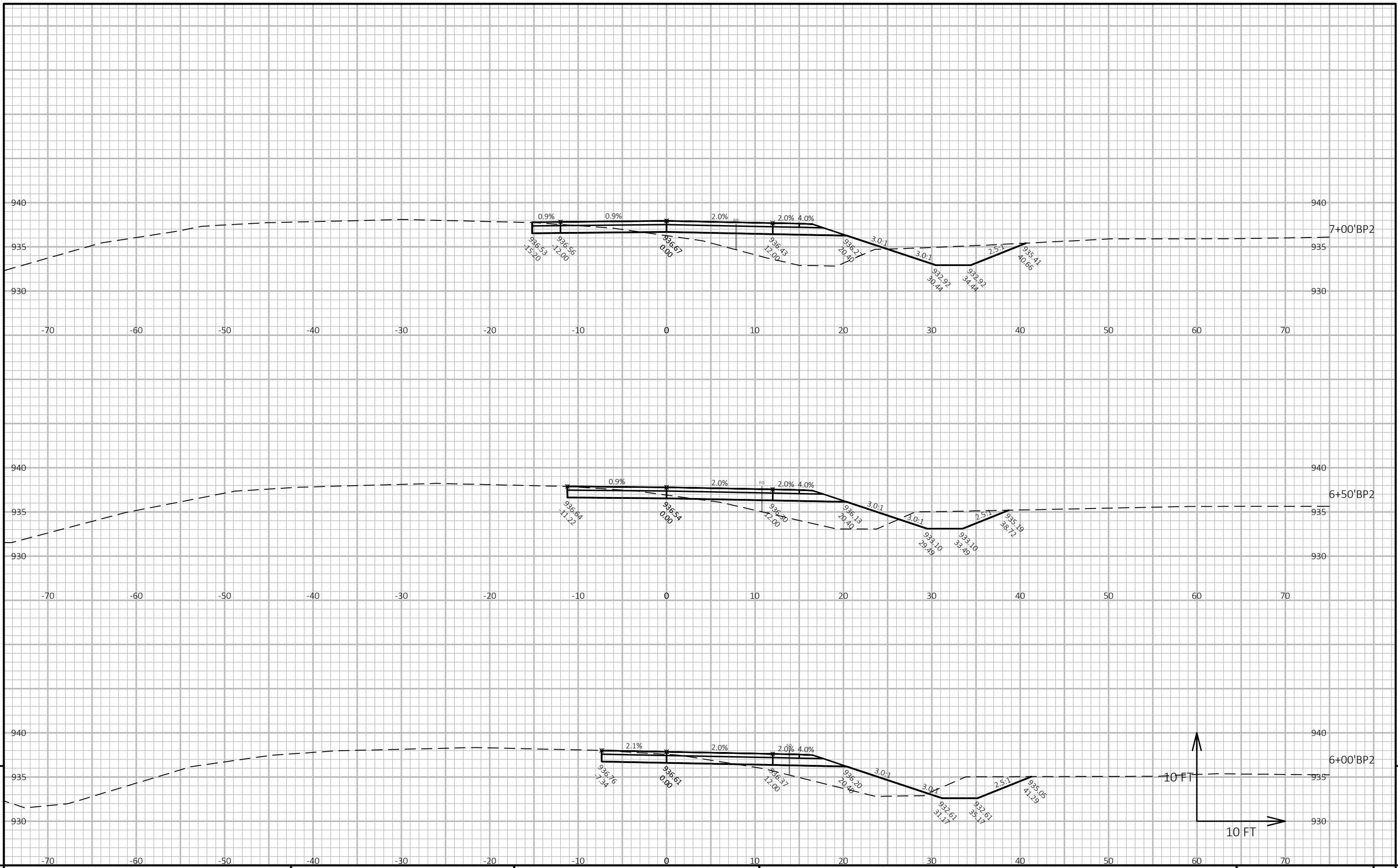
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FILE NAME : \\EAURTOPFLPPI01\N3PUBLIC\PD\3\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_BP.DWG PLOT DATE : 11/1/2019 9:56 AM PLOT BY : PAYNE, MATTHEW A PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

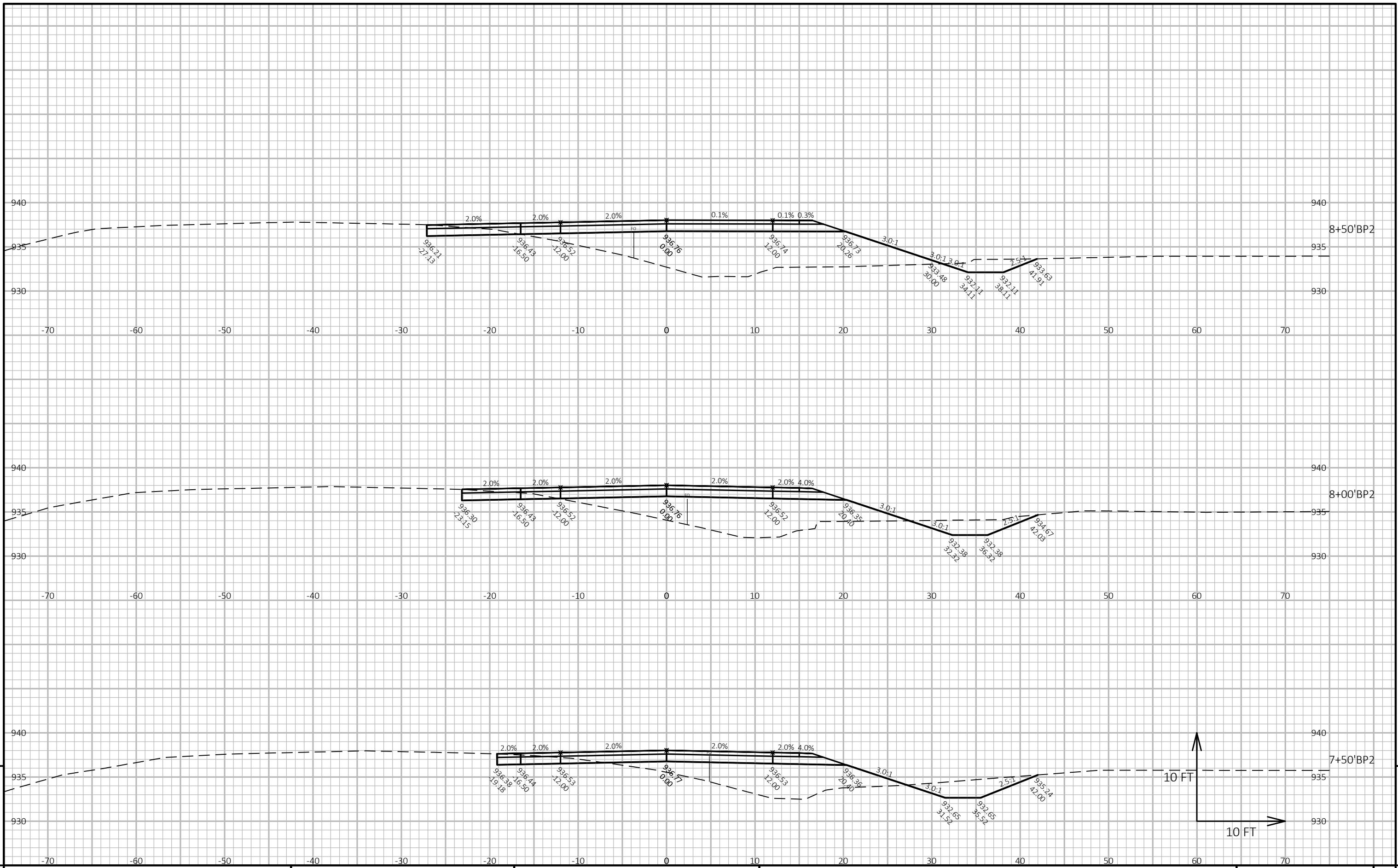
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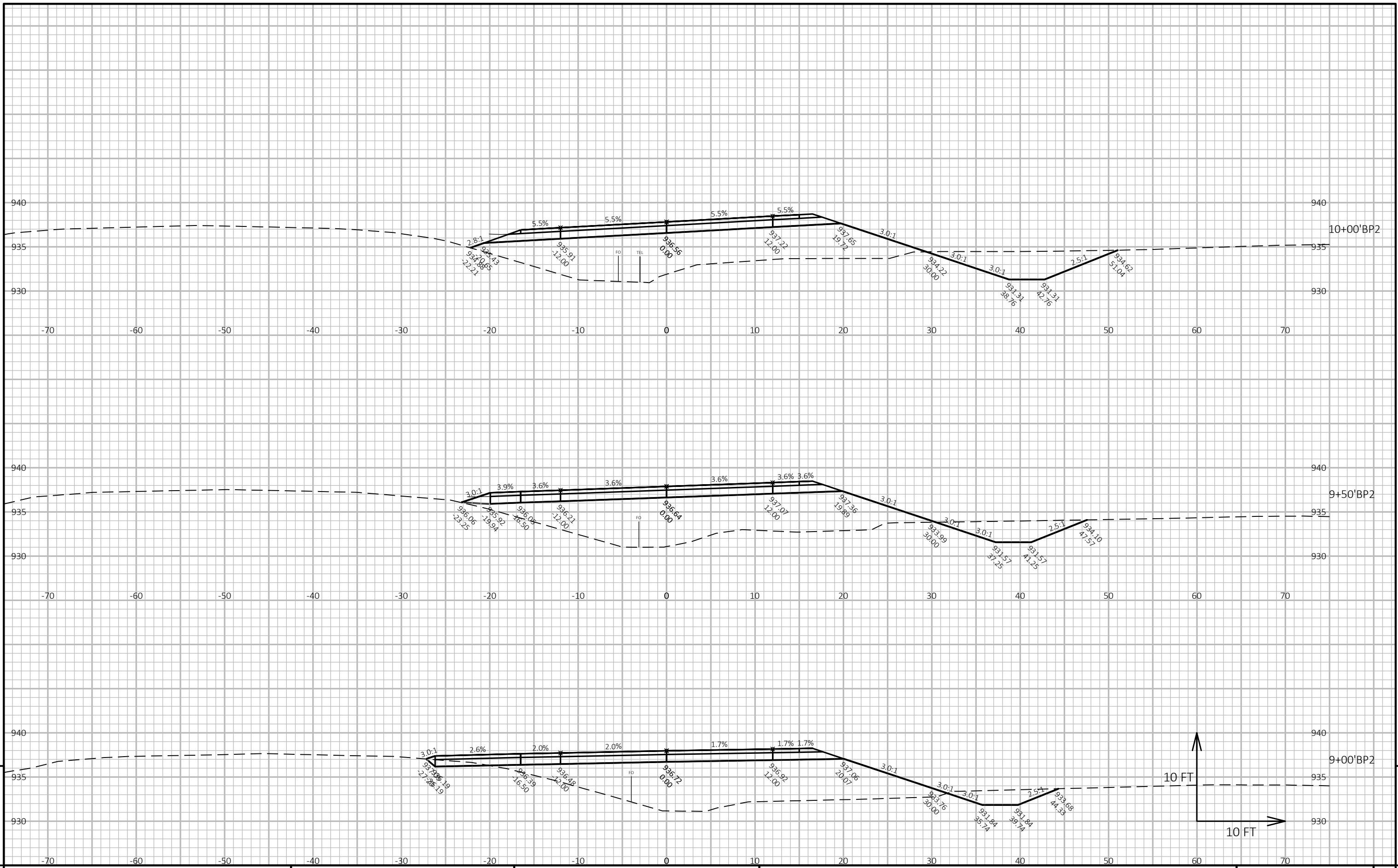
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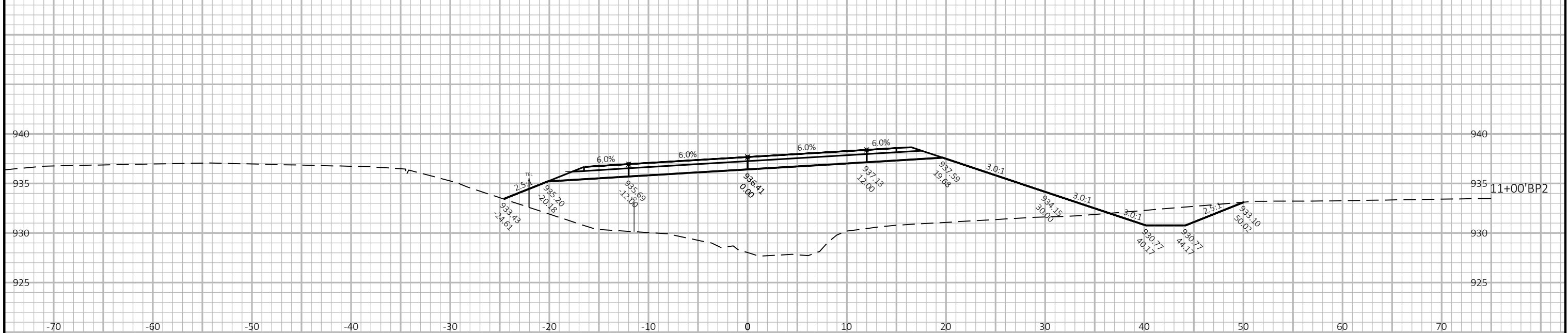
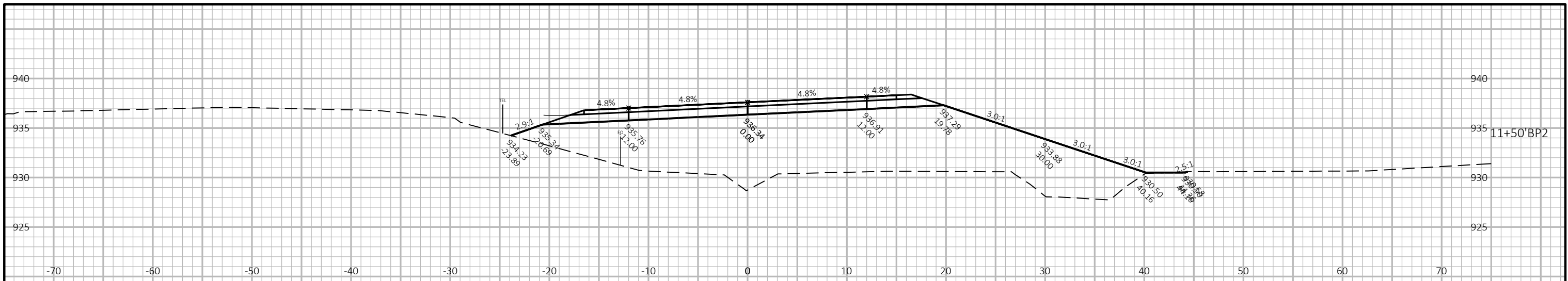
PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E



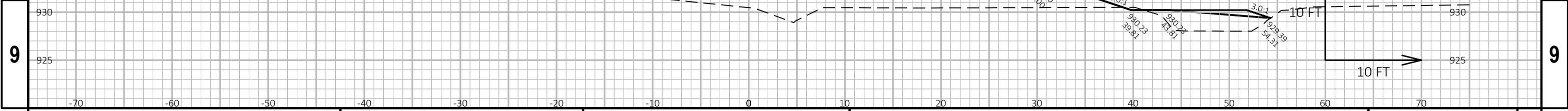
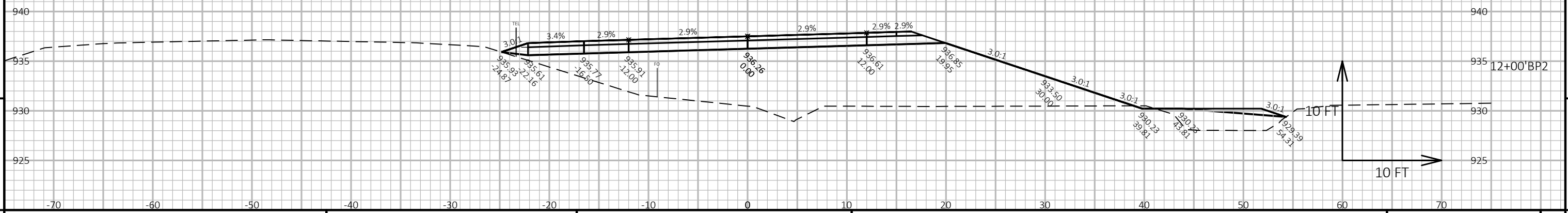
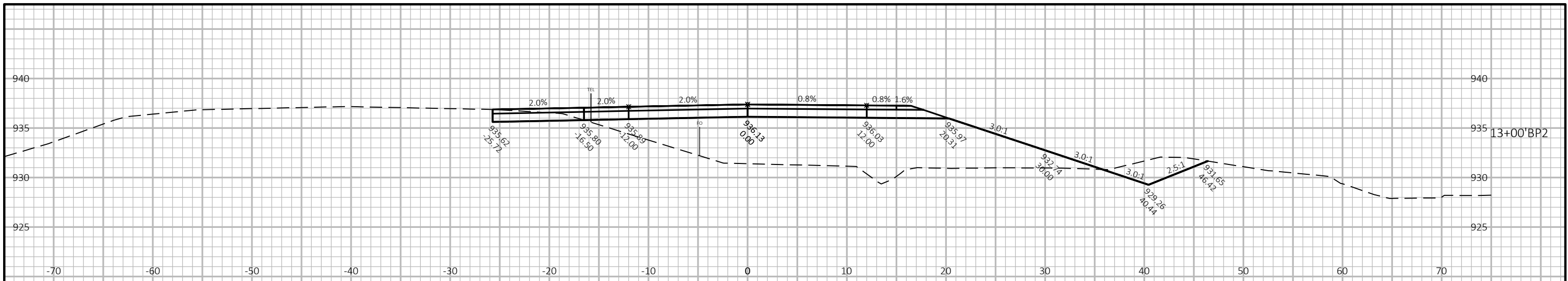
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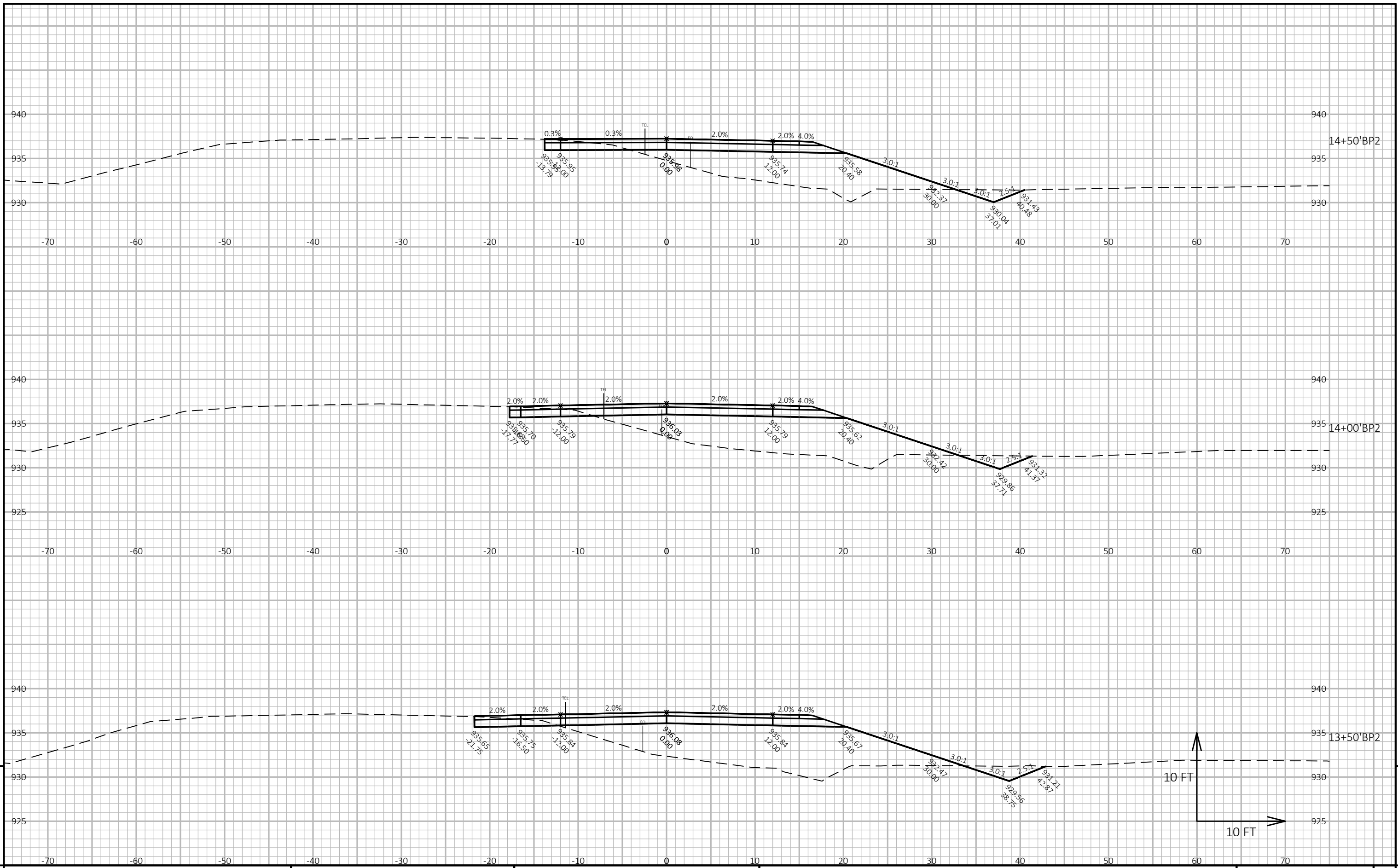
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E



PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E



PROJECT NO: 7080-01-72

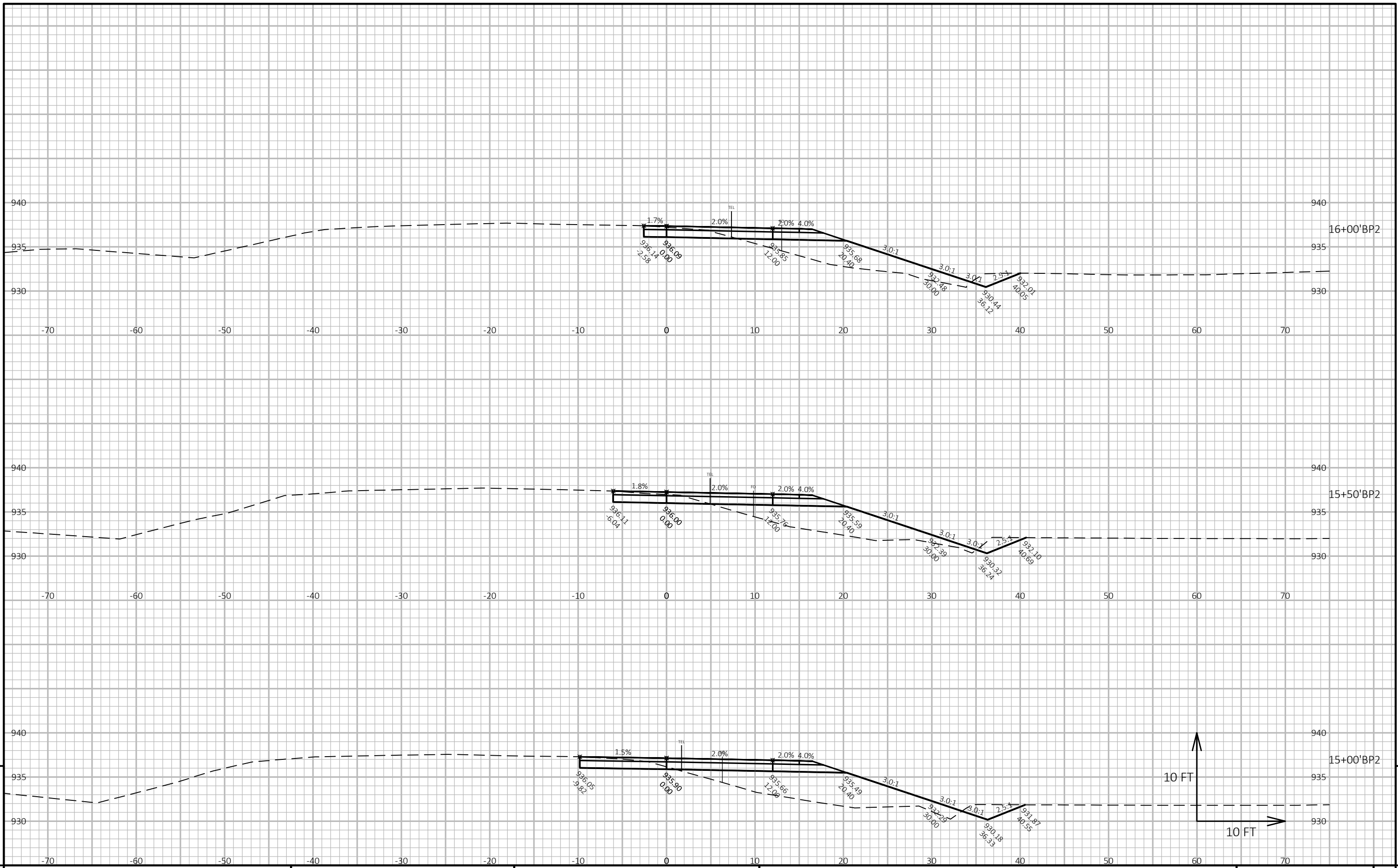
HWY: USH 12

COUNTY: EAU CLAIRE

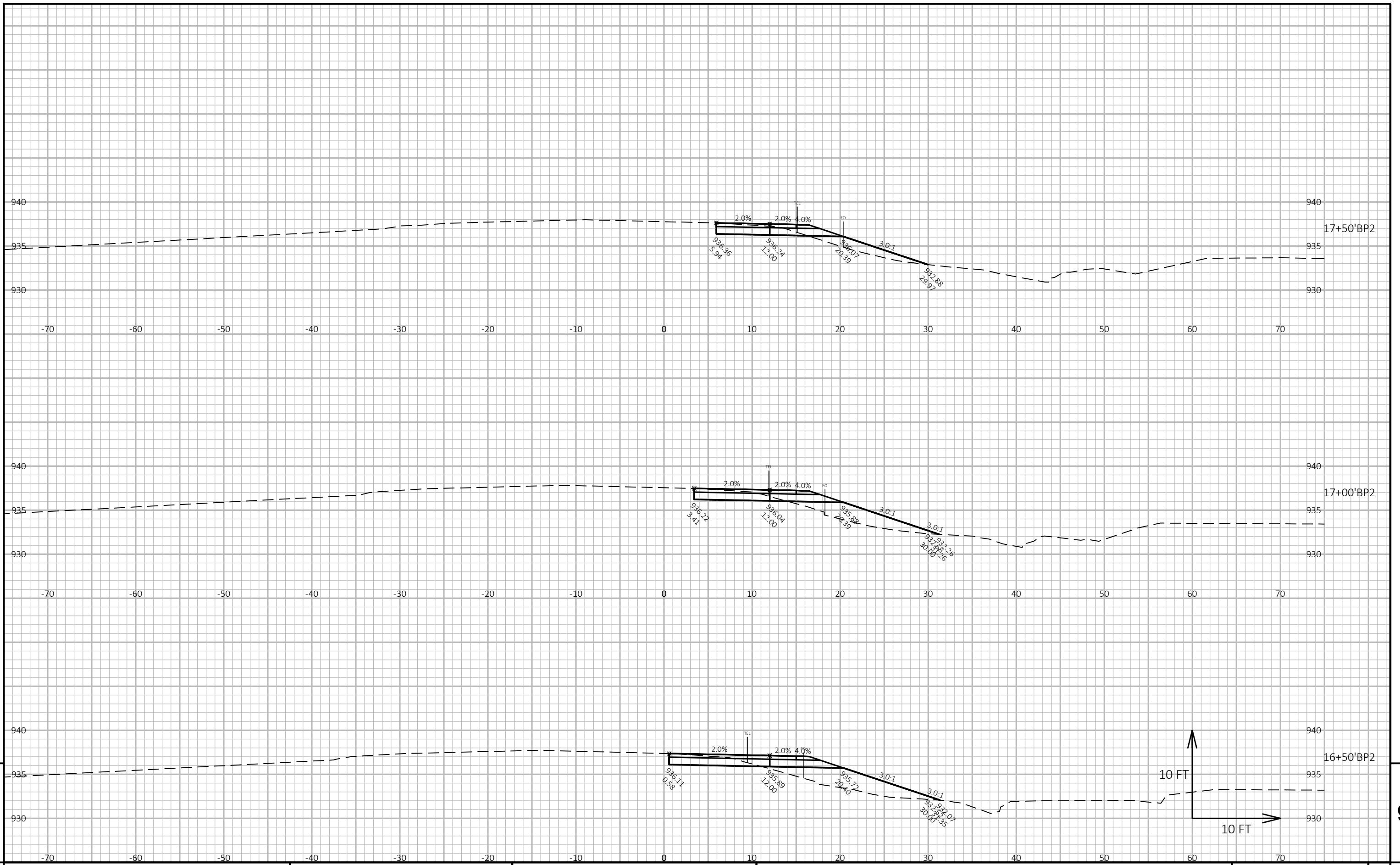
CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS

SHEET

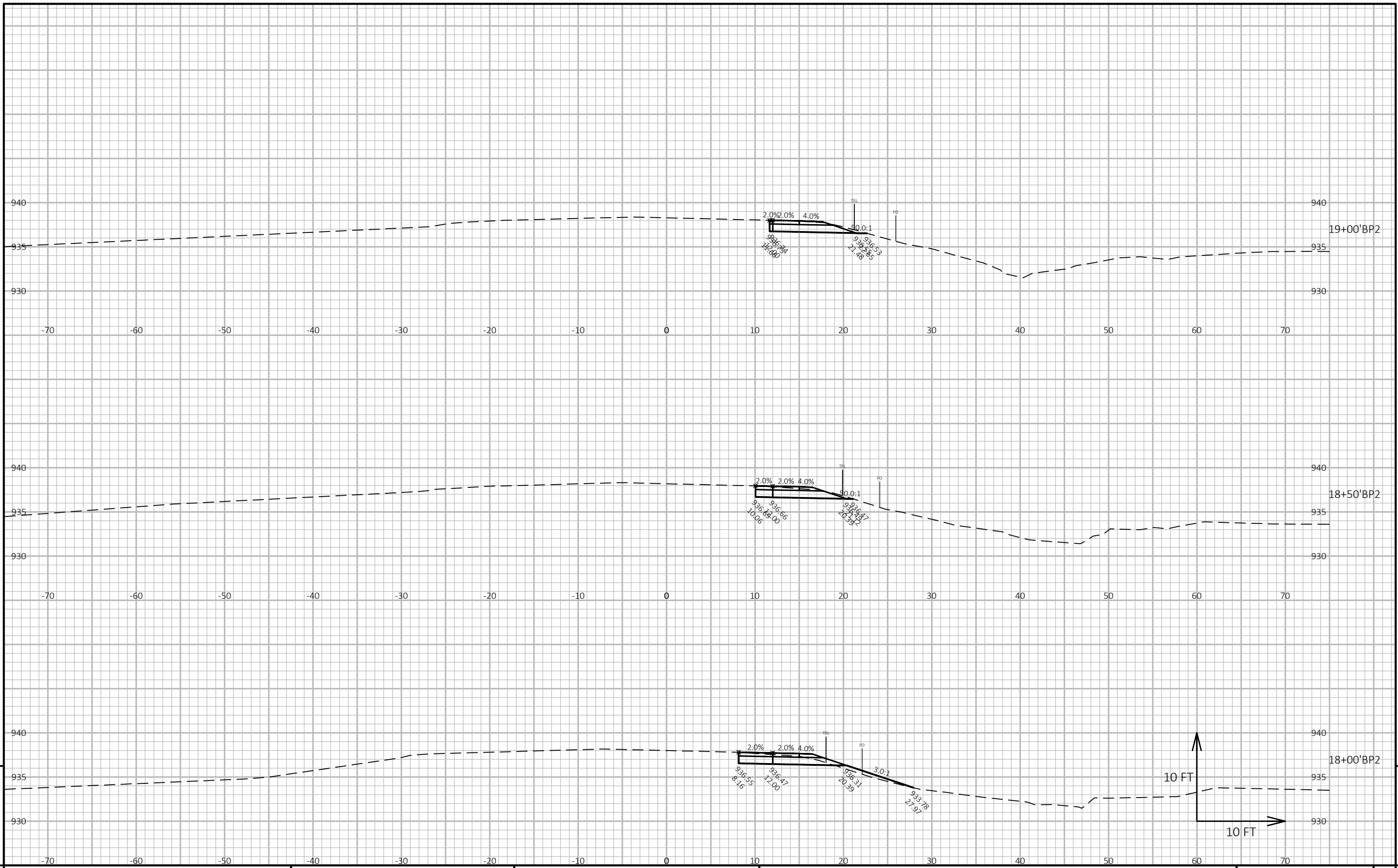
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E



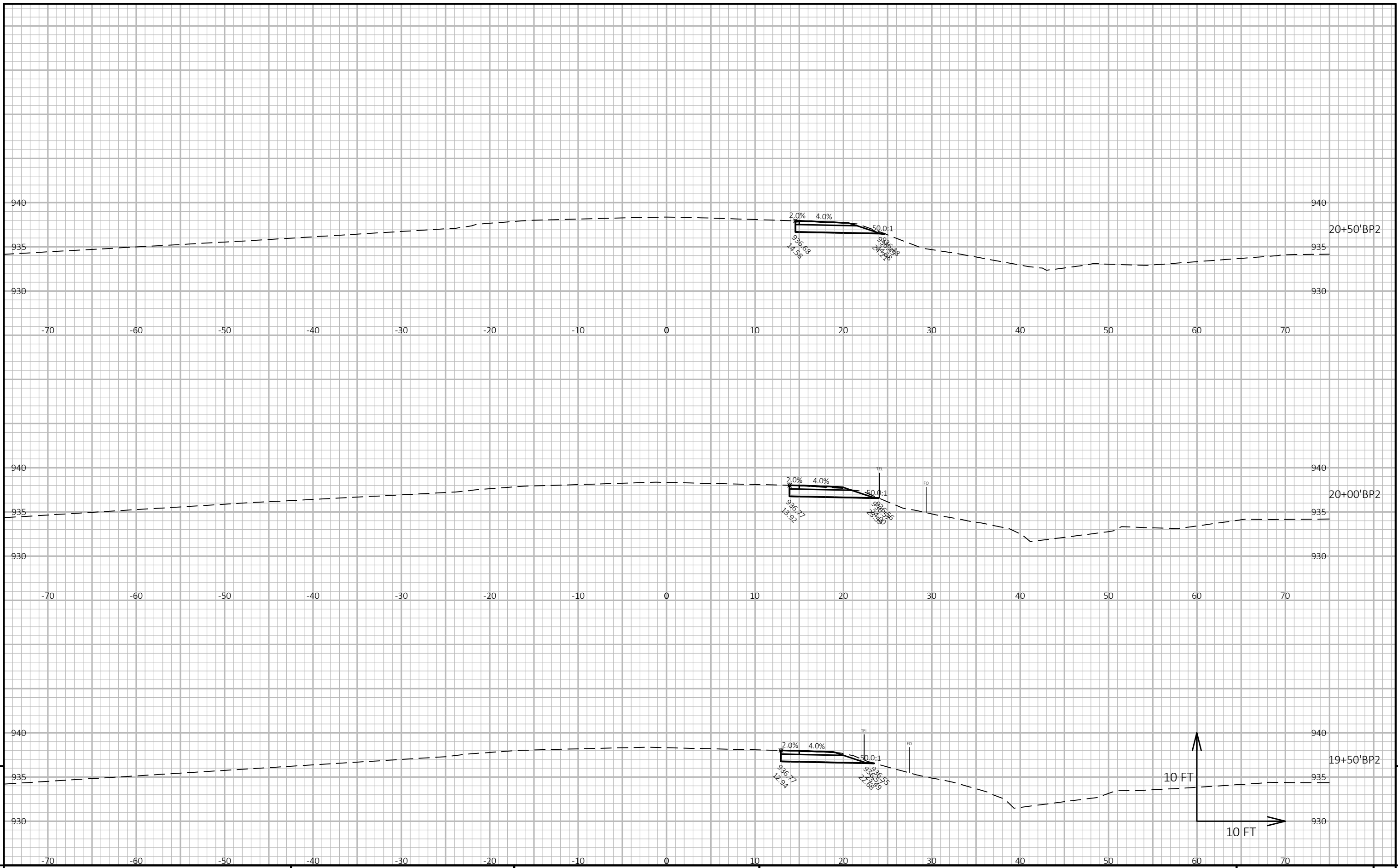
PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E



PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS	SHEET
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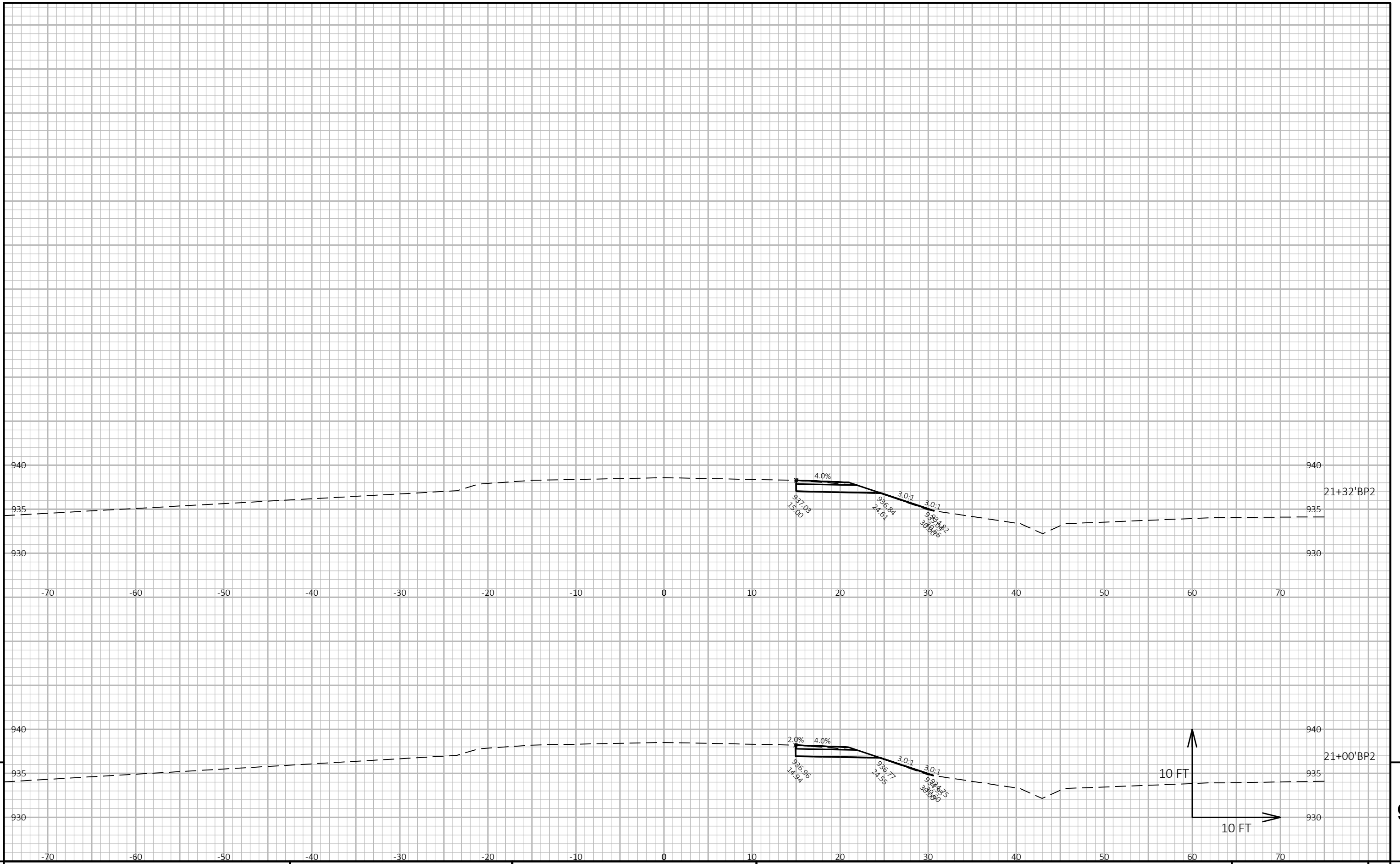


PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E

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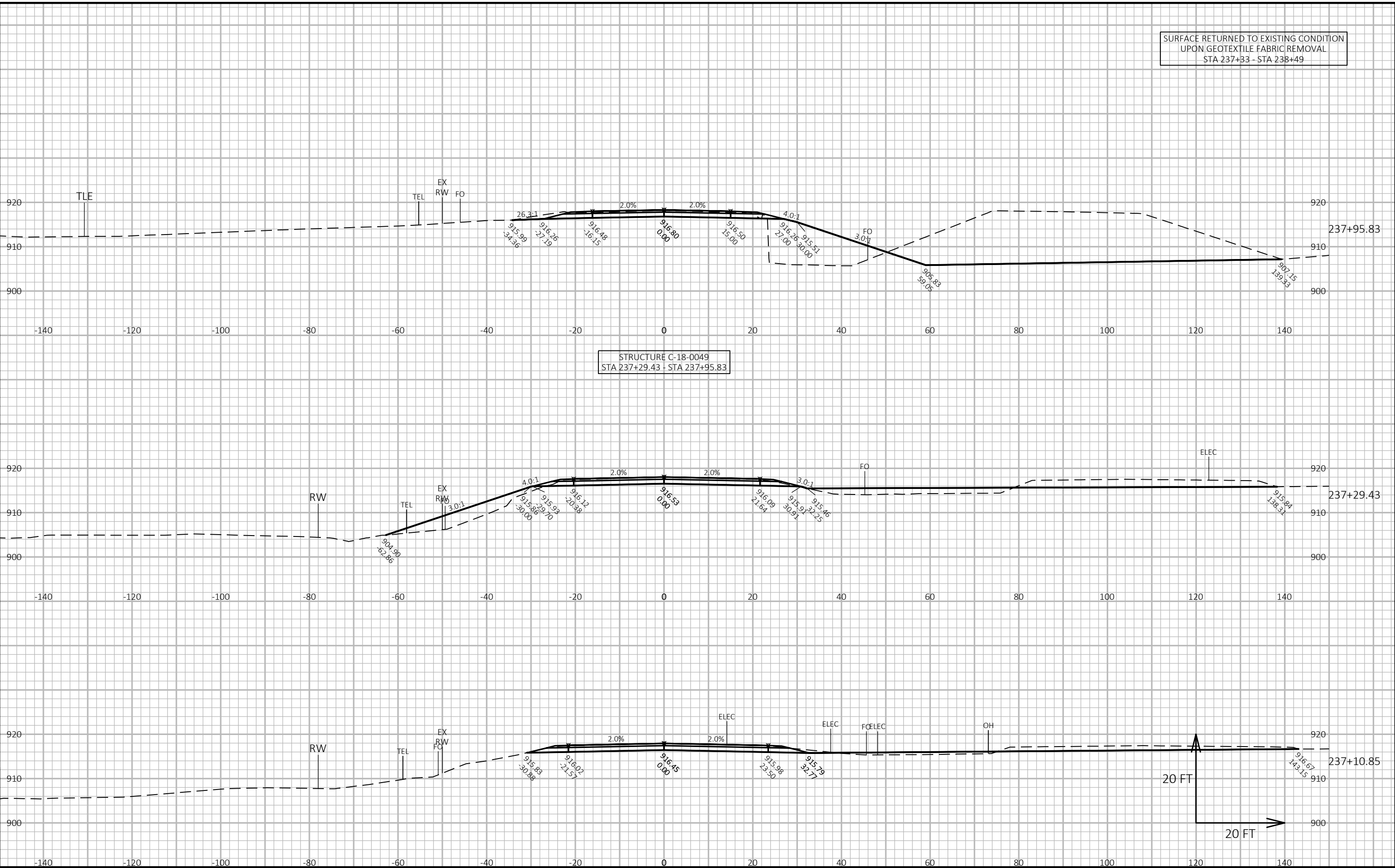
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: NINE MILE CREEK TEMPORARY BYPASS SHEET E

FILE NAME : \\EAURTOPFLPPI01\N3PUBLIC\PDS\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_BP.DWG PLOT DATE : 11/1/2019 9:56 AM PLOT BY : PAYNE, MATTHEW A PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 237+33 - STA 238+49



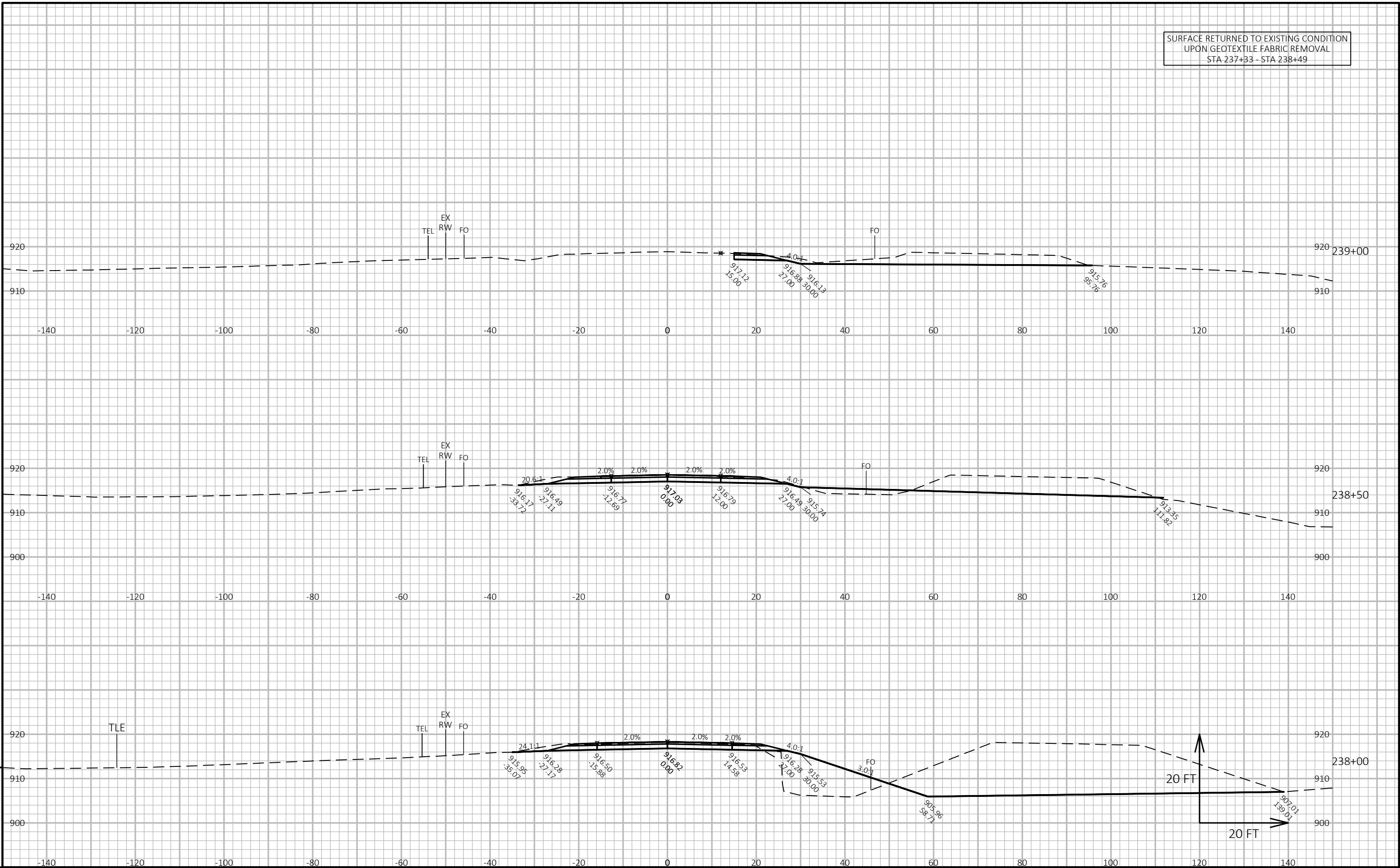
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

FILE NAME: \\EAURTOPFLPPI01\N3PUBLIC\PDS\C3D\70800102\SHEETPLAN\090200-CROSS SECTIONS\090200_XS_USH12.DWG PLOT DATE: 10/31/2019 9:17 AM PLOT BY: PAYNE, MATTHEW A PLOT NAME: PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT. WISDOT/CADD SHEET 49

SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 237+33 - STA 238+49



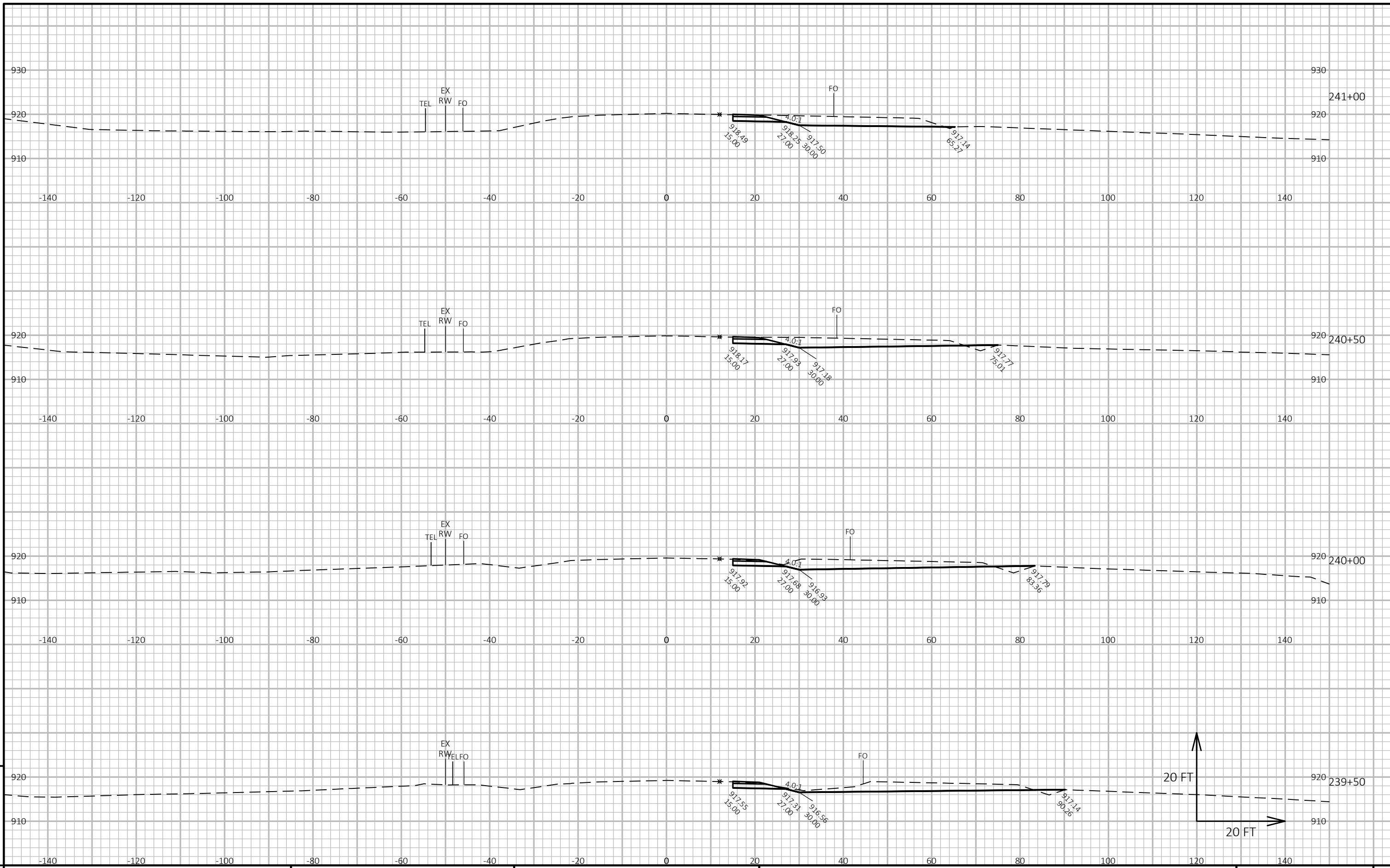
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

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LAYOUT NAME - 090225



PROJECT NO: 7080-01-72

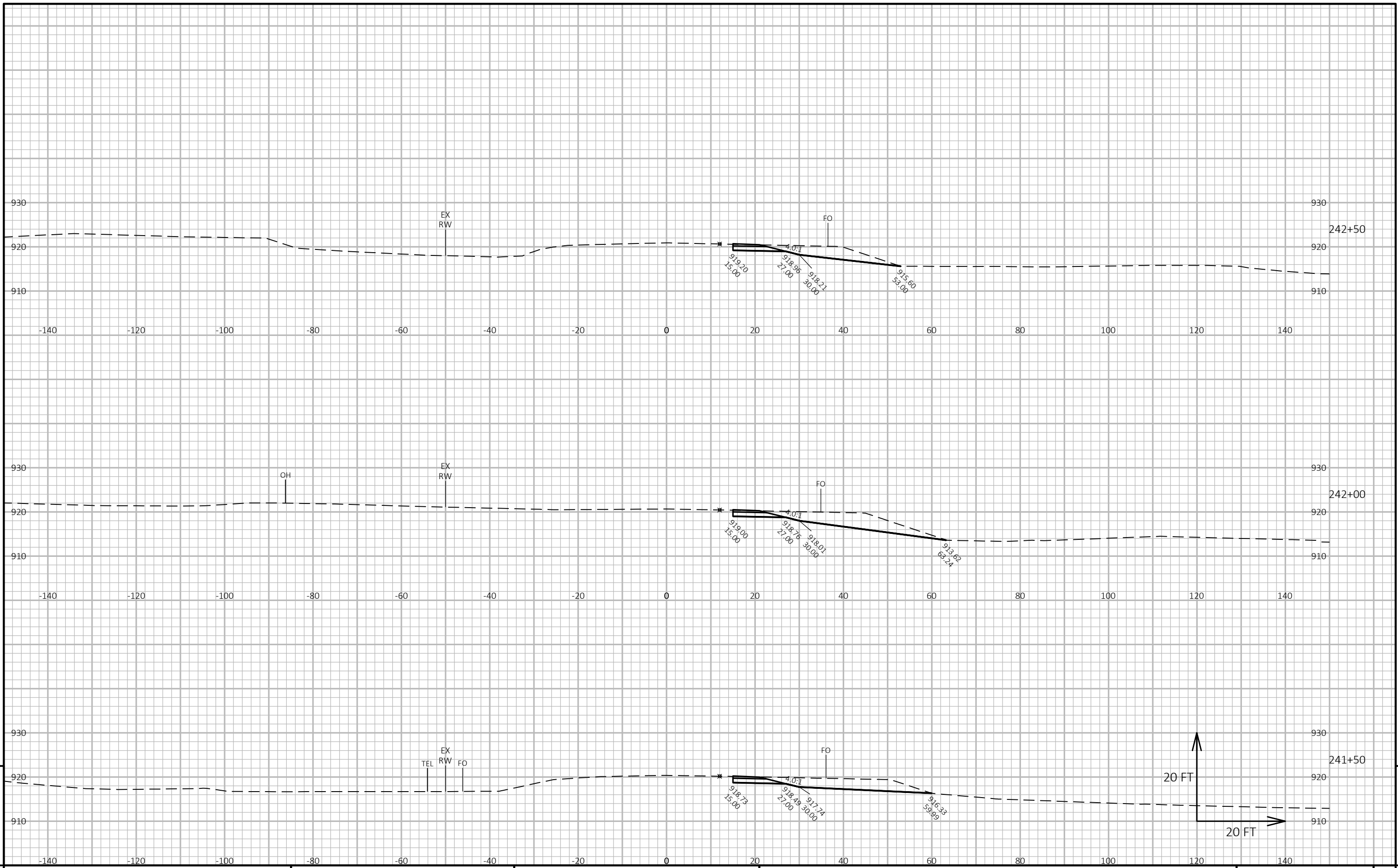
HWY: USH 12

COUNTY: EAU CLAIRE

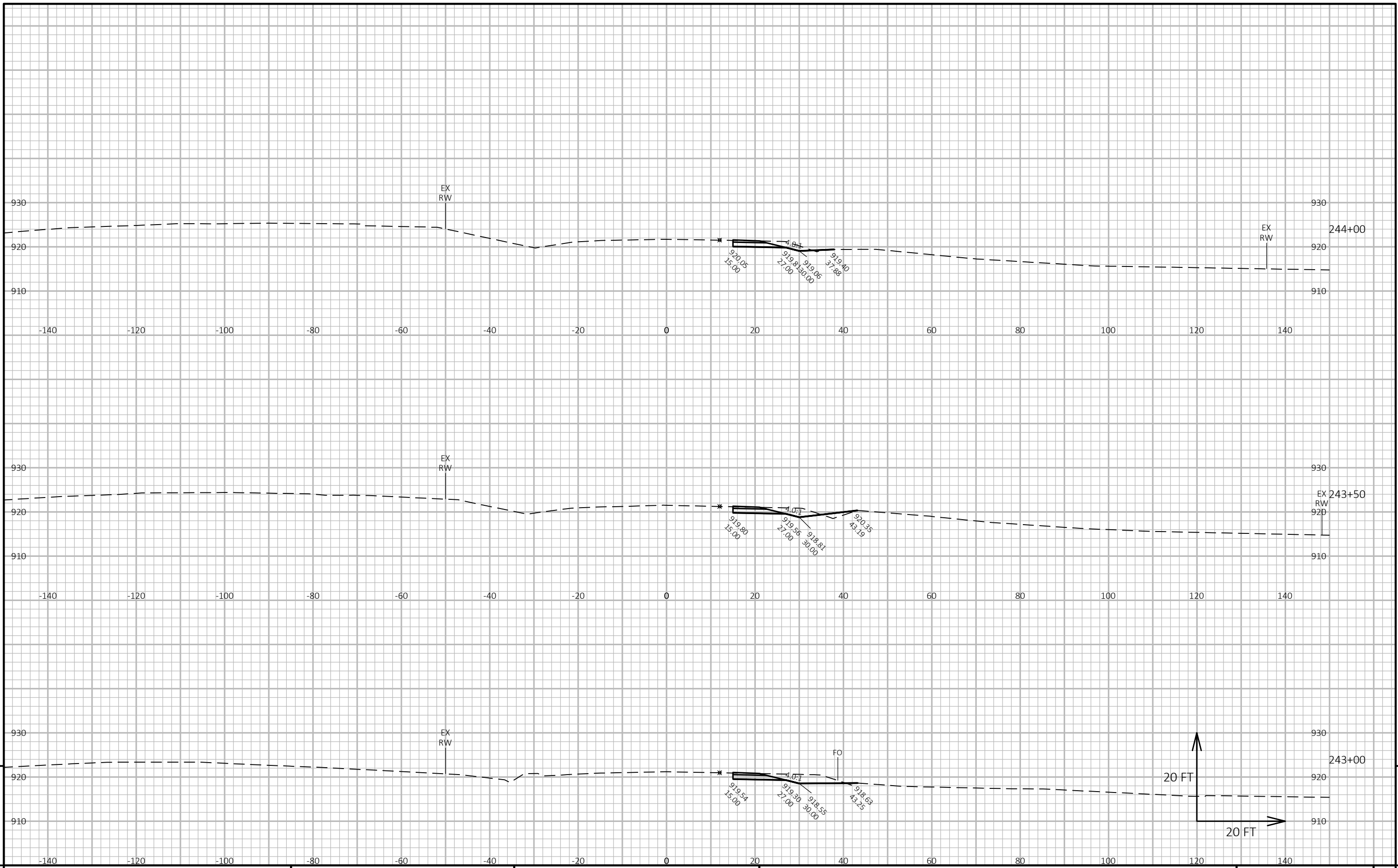
CROSS SECTIONS: STAGE 3 - USH 12

SHEET

E



PROJECT NO: 7080-01-72	HWY: USH 12	COUNTY: EAU CLAIRE	CROSS SECTIONS: STAGE 3 - USH 12	SHEET	E
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PROJECT NO: 7080-01-72

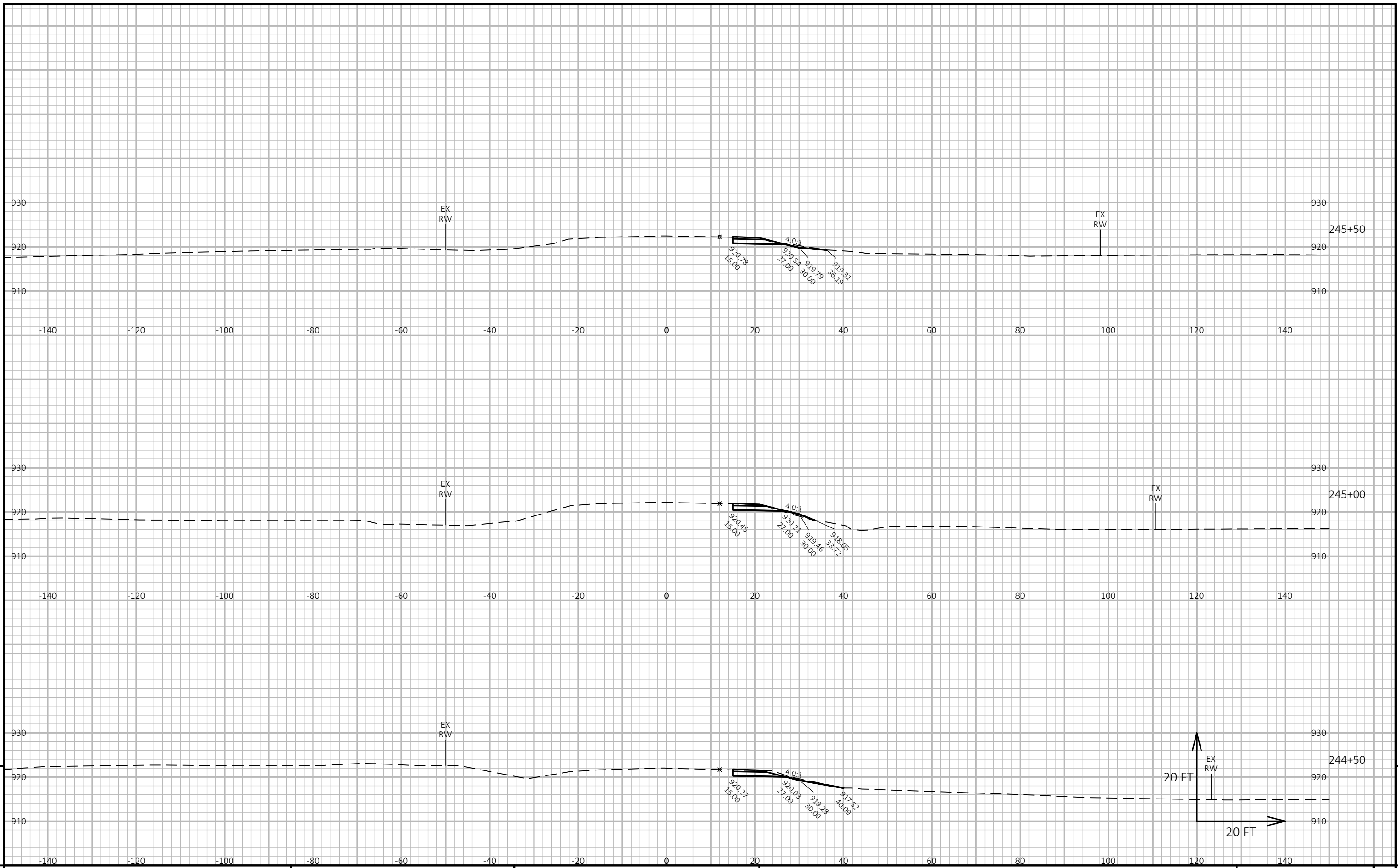
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COUNTY: EAU CLAIRE

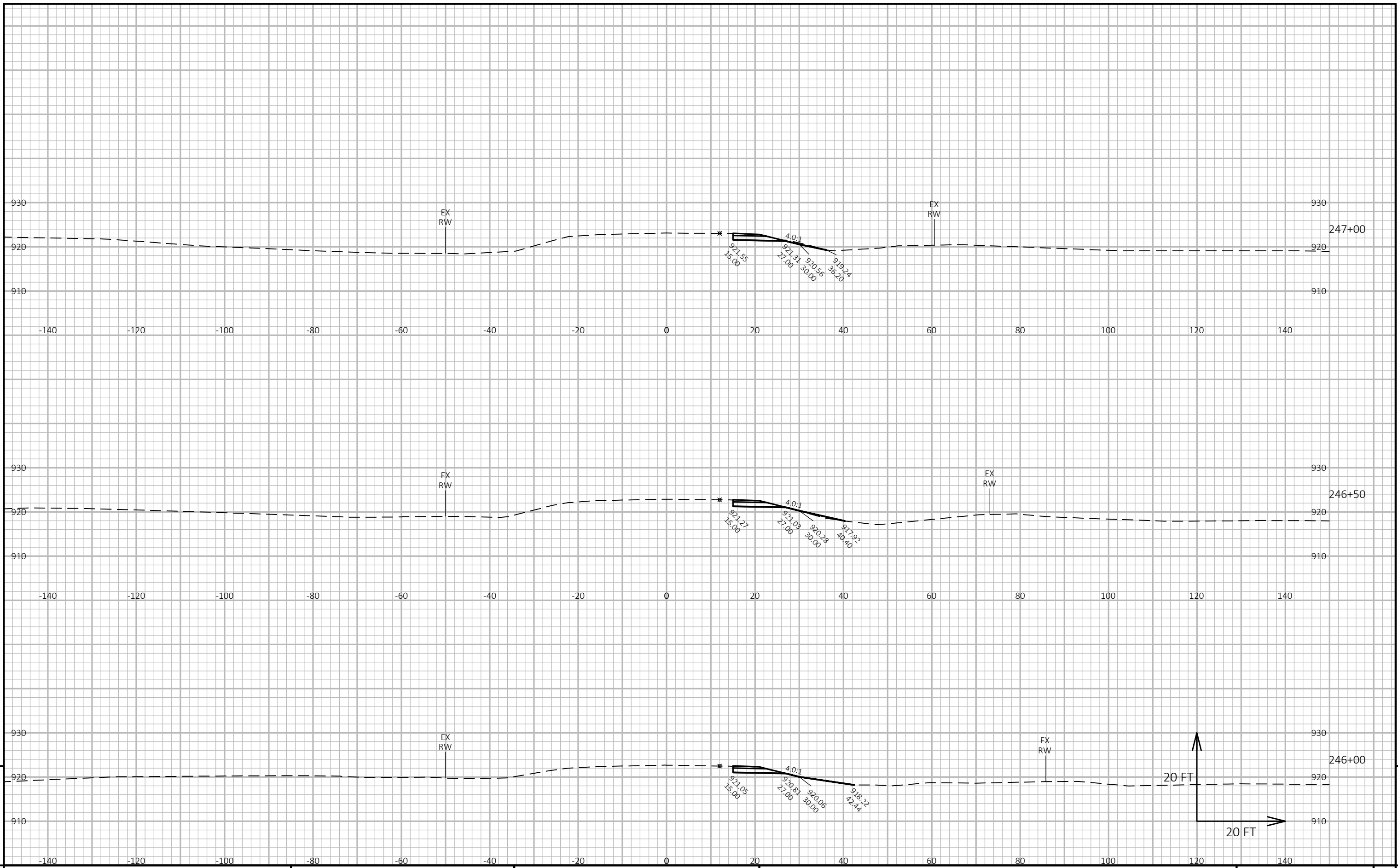
CROSS SECTIONS: STAGE 3 - USH 12

SHEET

E



PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

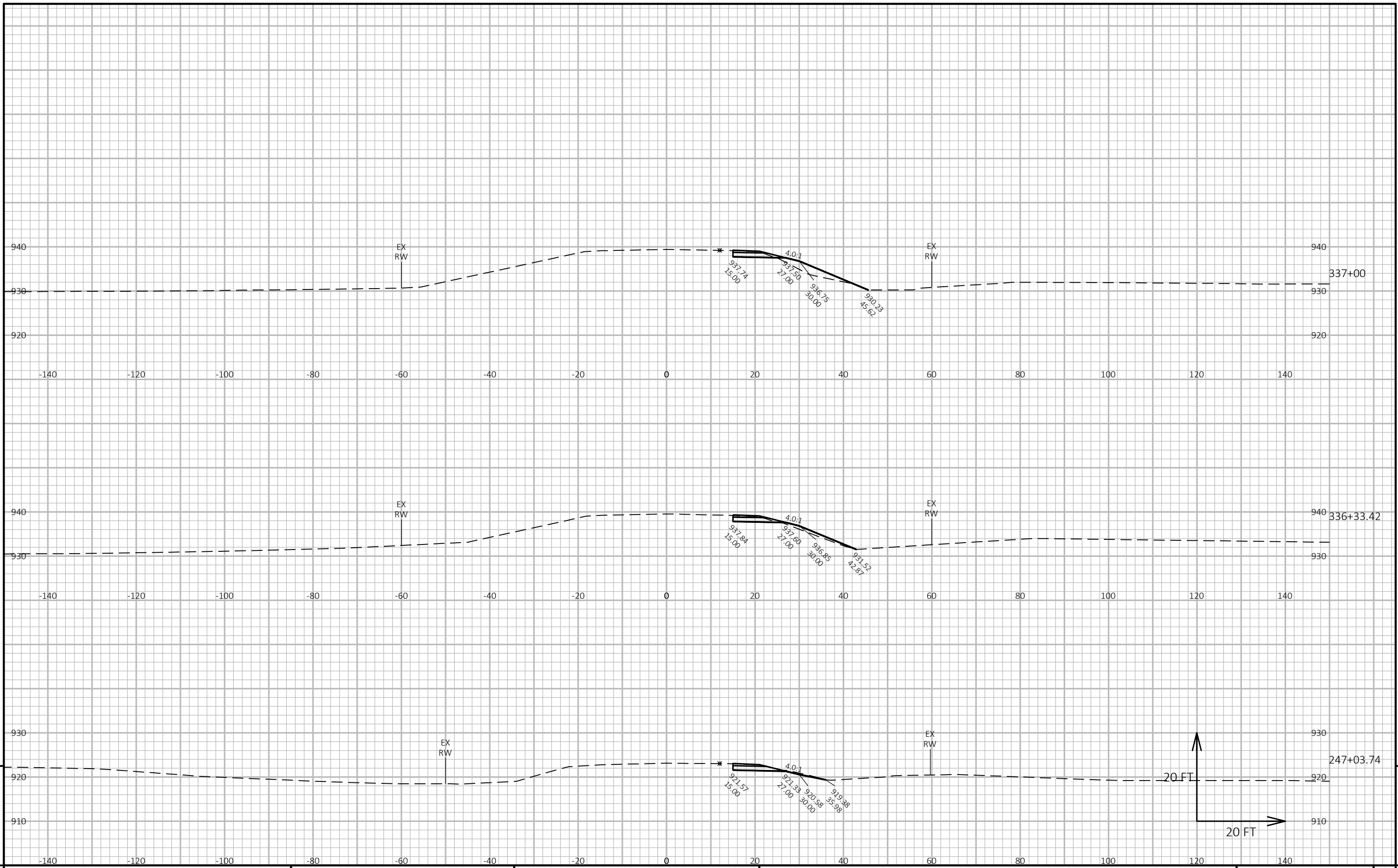


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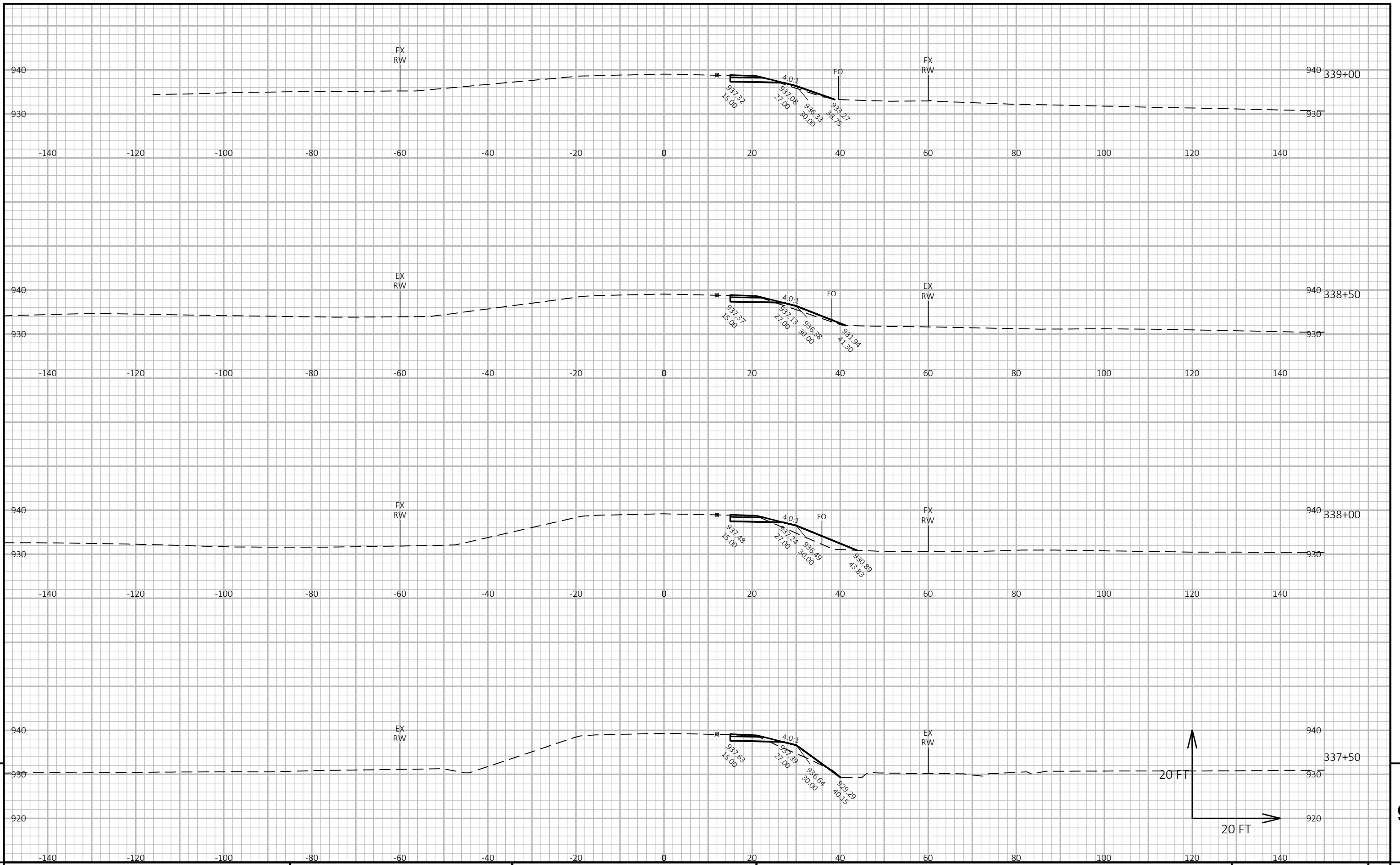
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

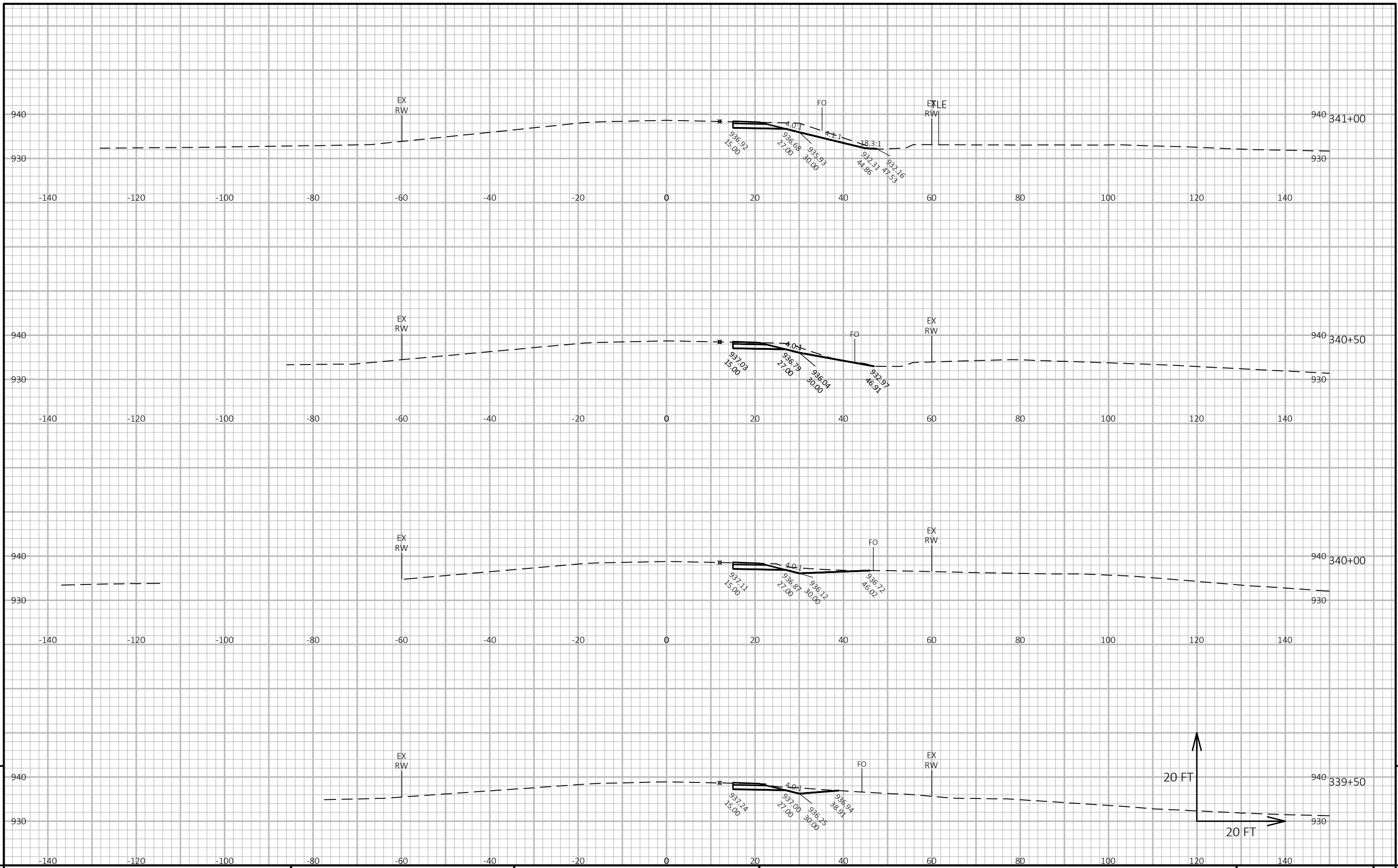
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E



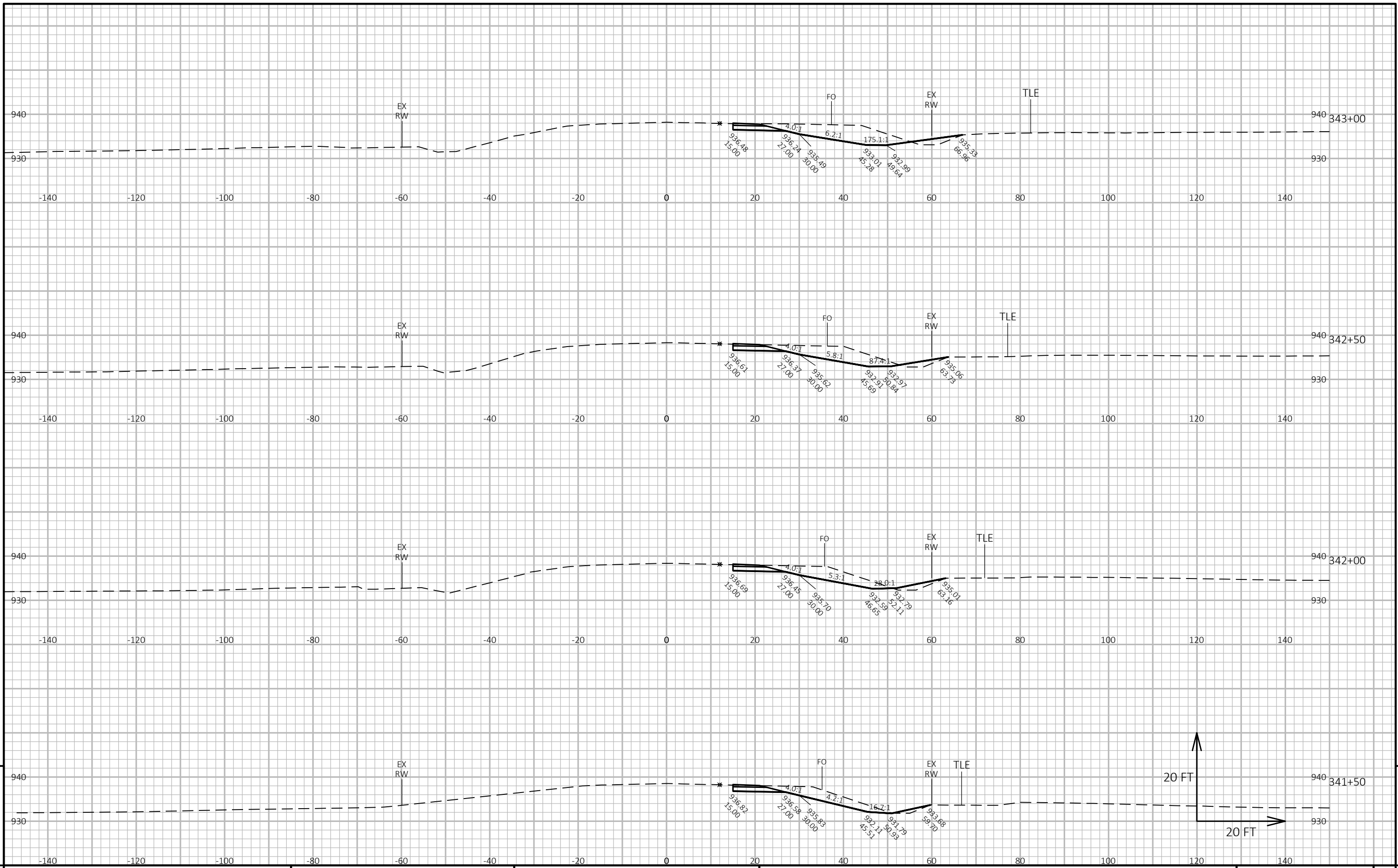
PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E



PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

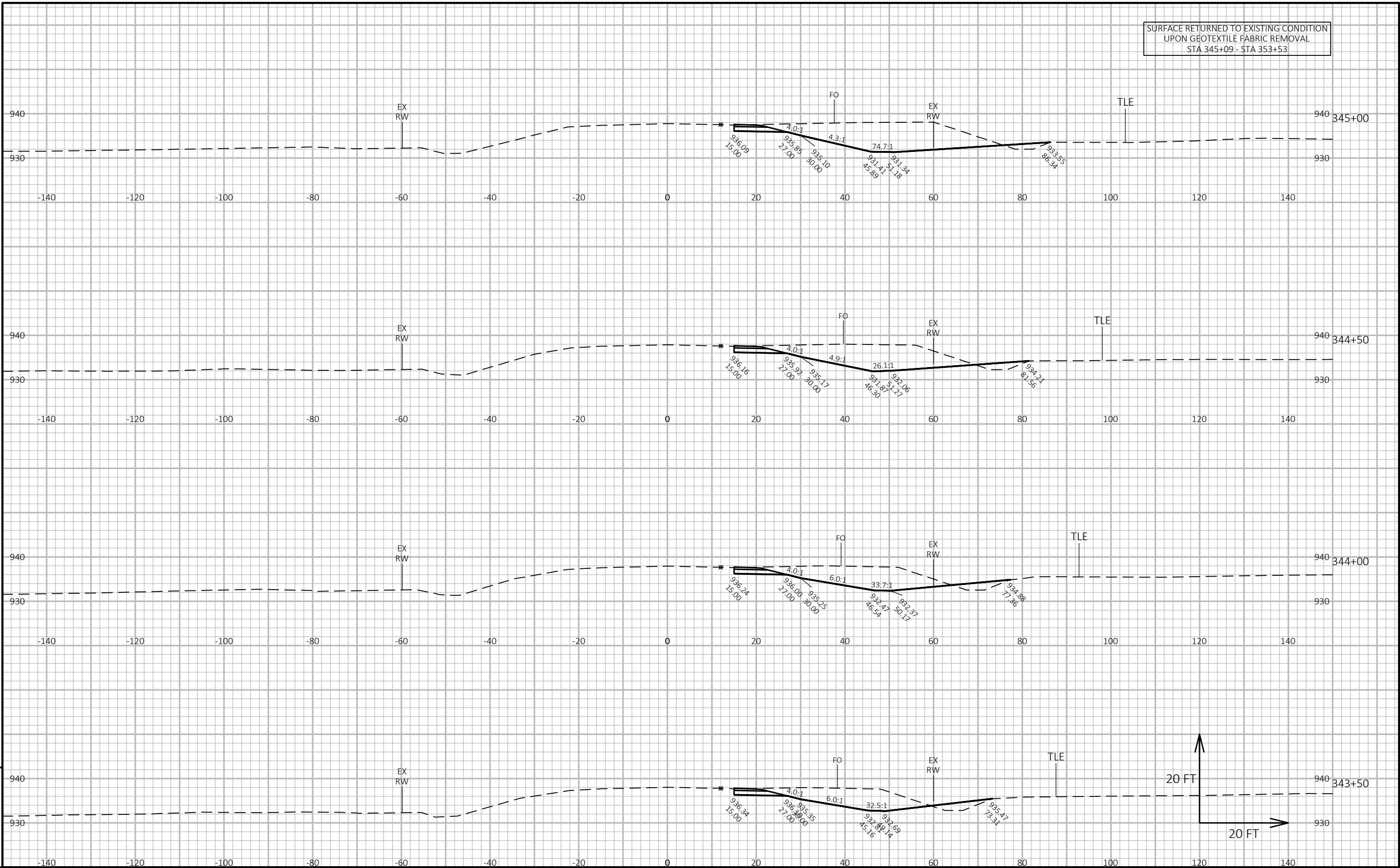
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 345+09 - STA 353+53



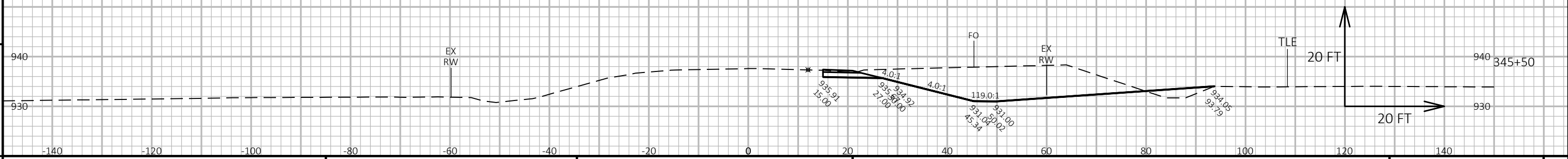
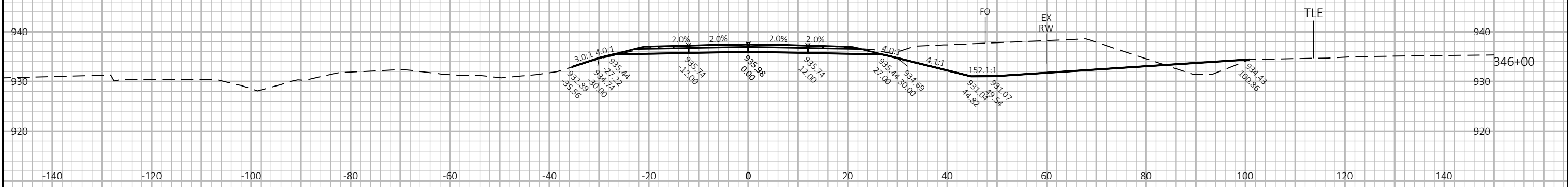
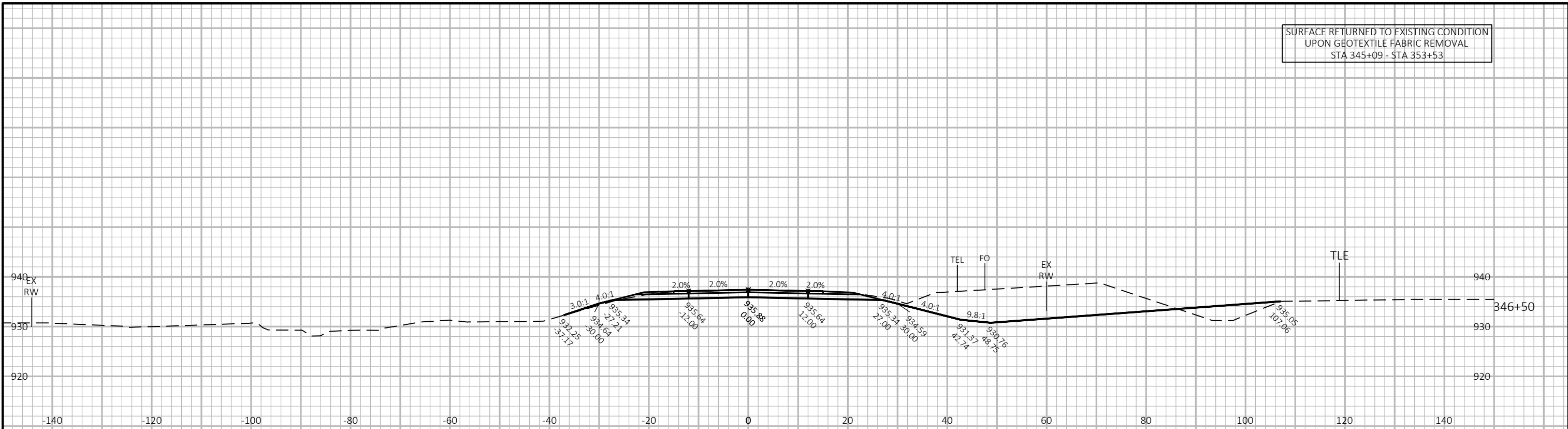
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

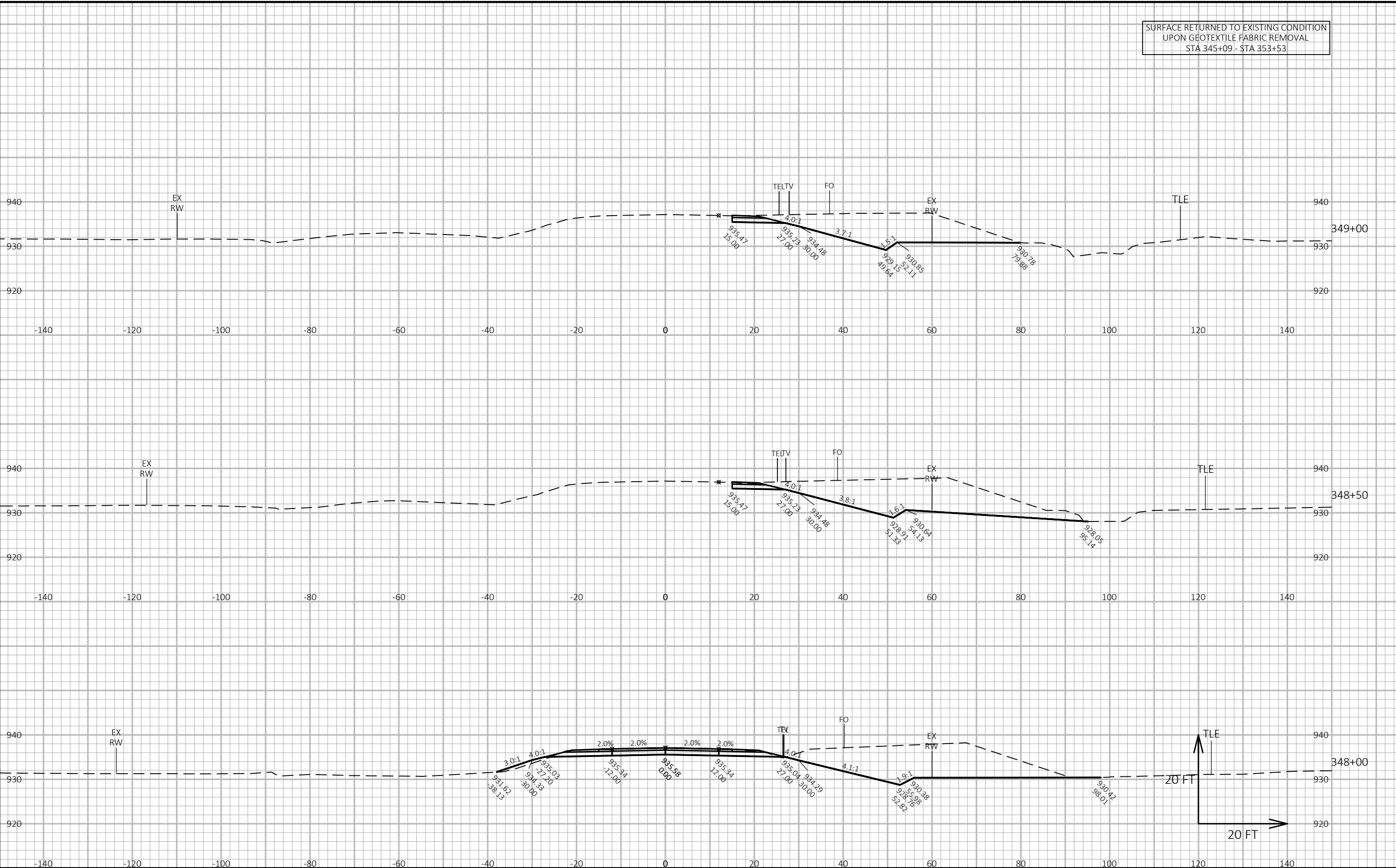
FILE NAME: \\EAURTOP\PLP\I01\N3PUBLIC\PDS\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_USH12.DWG PLOT DATE: 10/31/2019 9:18 AM PLOT BY: PAYNE, MATTHEW A PLOT NAME: PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT. WISDOT/CADD SHEET 49

SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 345+09 - STA 353+53



PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET 9

SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 345+09 - STA 353+53



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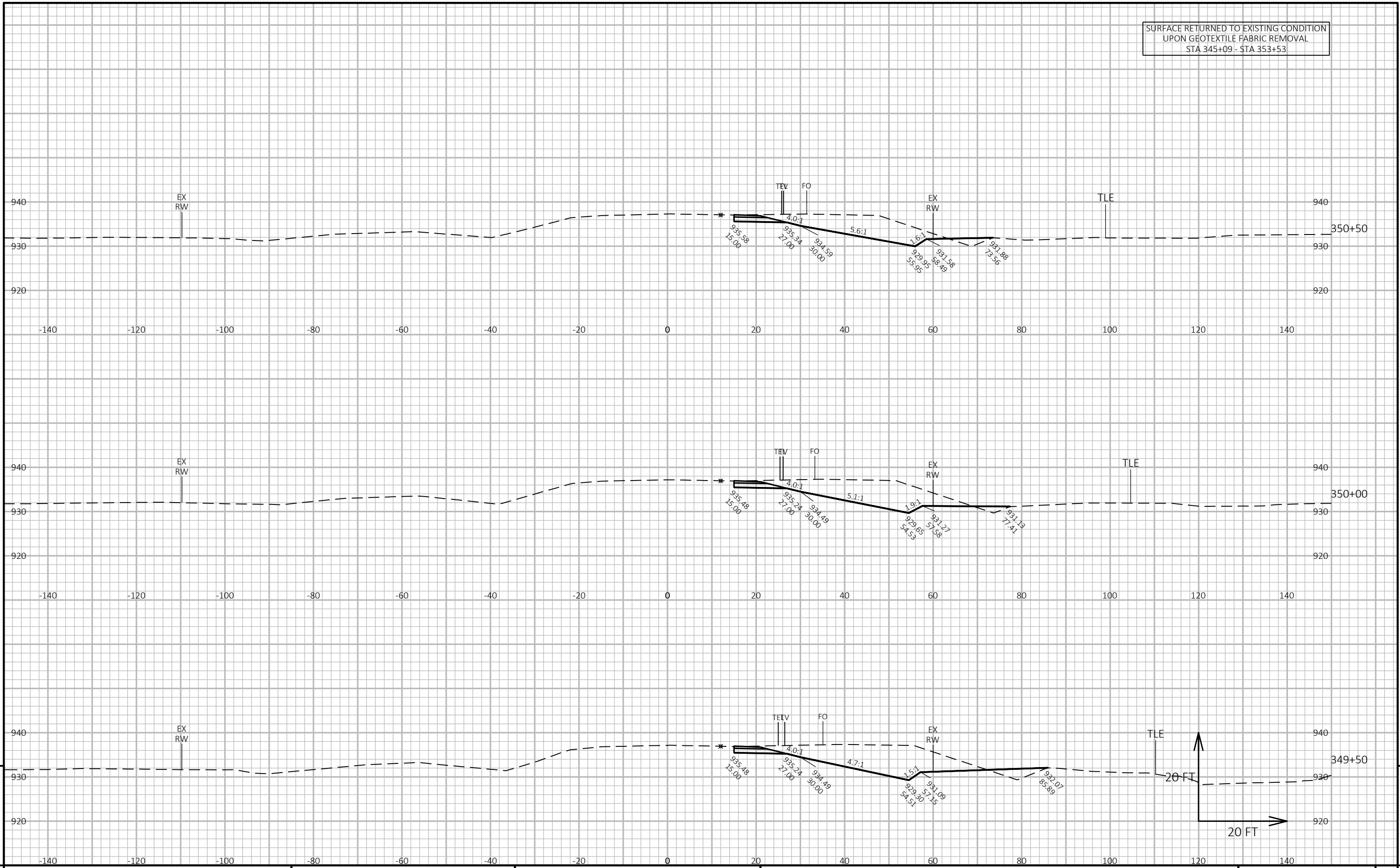
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

FILE NAME: \\EAURTOPFLPPI01\N3PUBLIC\PDS\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_USH12.DWG PLOT DATE: 10/31/2019 9:18 AM PLOT BY: PAYNE, MATTHEW A PLOT NAME: PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090238

SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 345+09 - STA 353+53



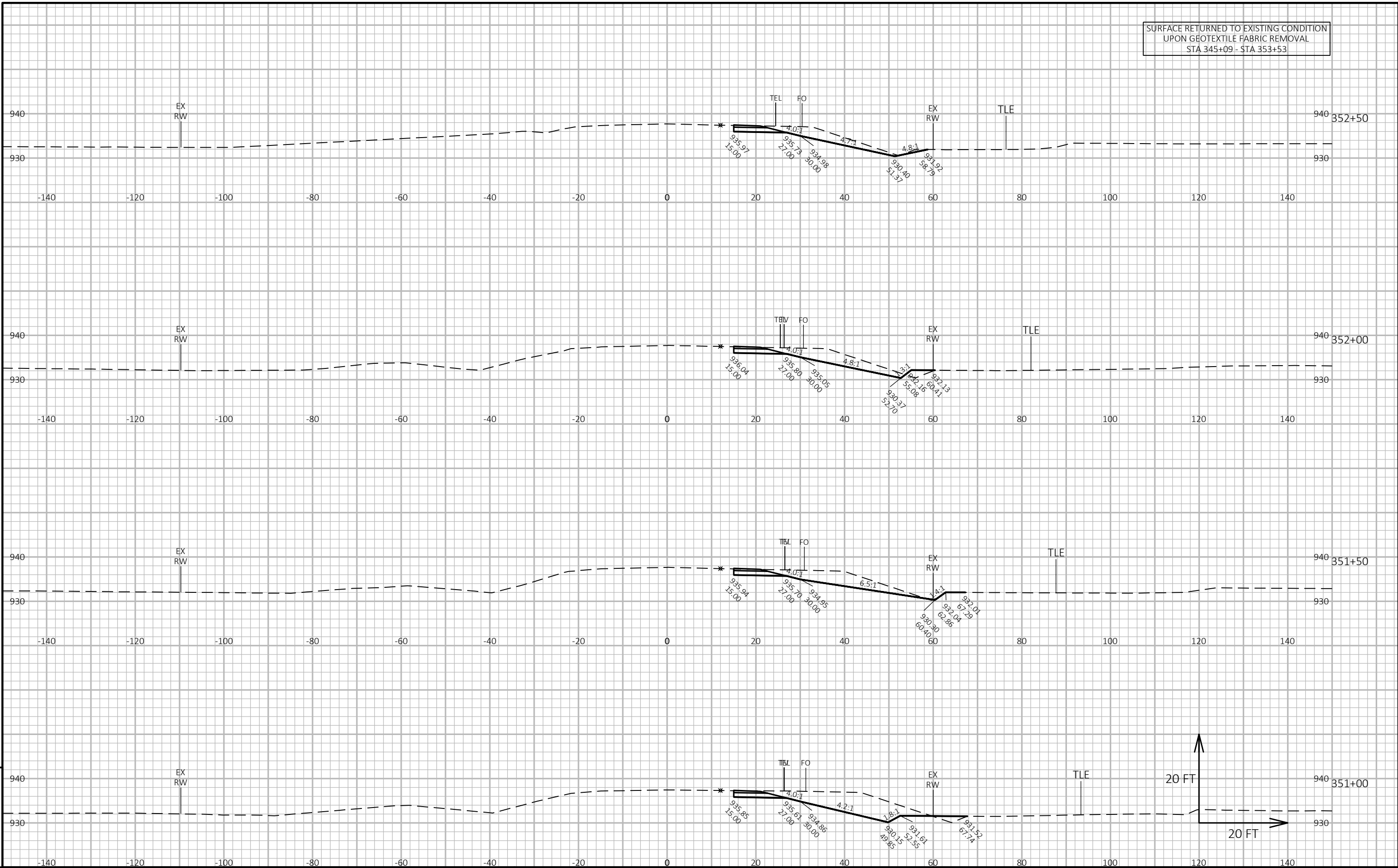
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PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E

FILE NAME: \\EAURTOPFLPPI01\N3PUBLIC\PDS\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_USH12.DWG PLOT DATE: 10/31/2019 9:18 AM PLOT BY: PAYNE, MATTHEW A PLOT NAME: PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT. WISDOT/CADD SHEET 49

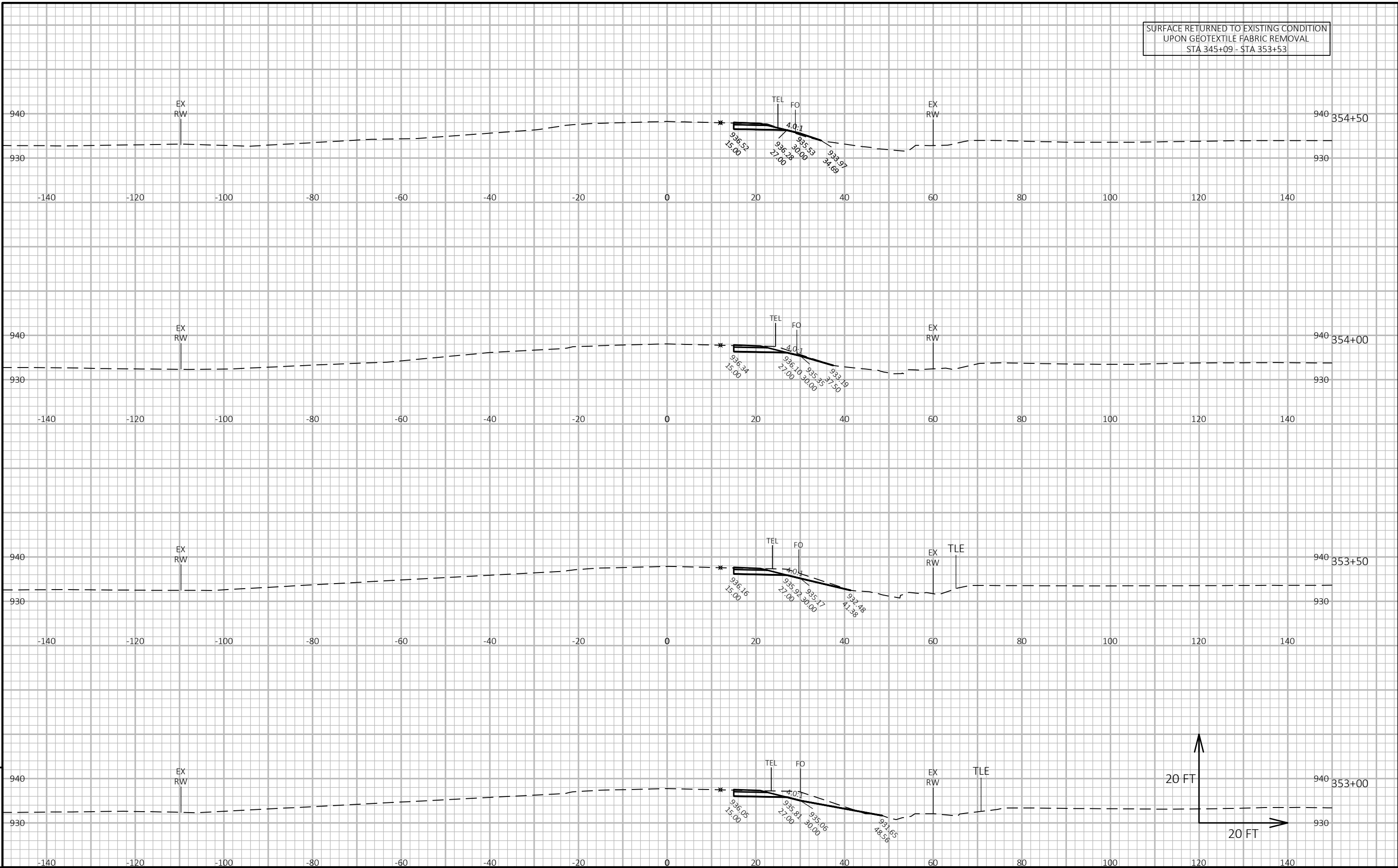
SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 345+09 - STA 353+53



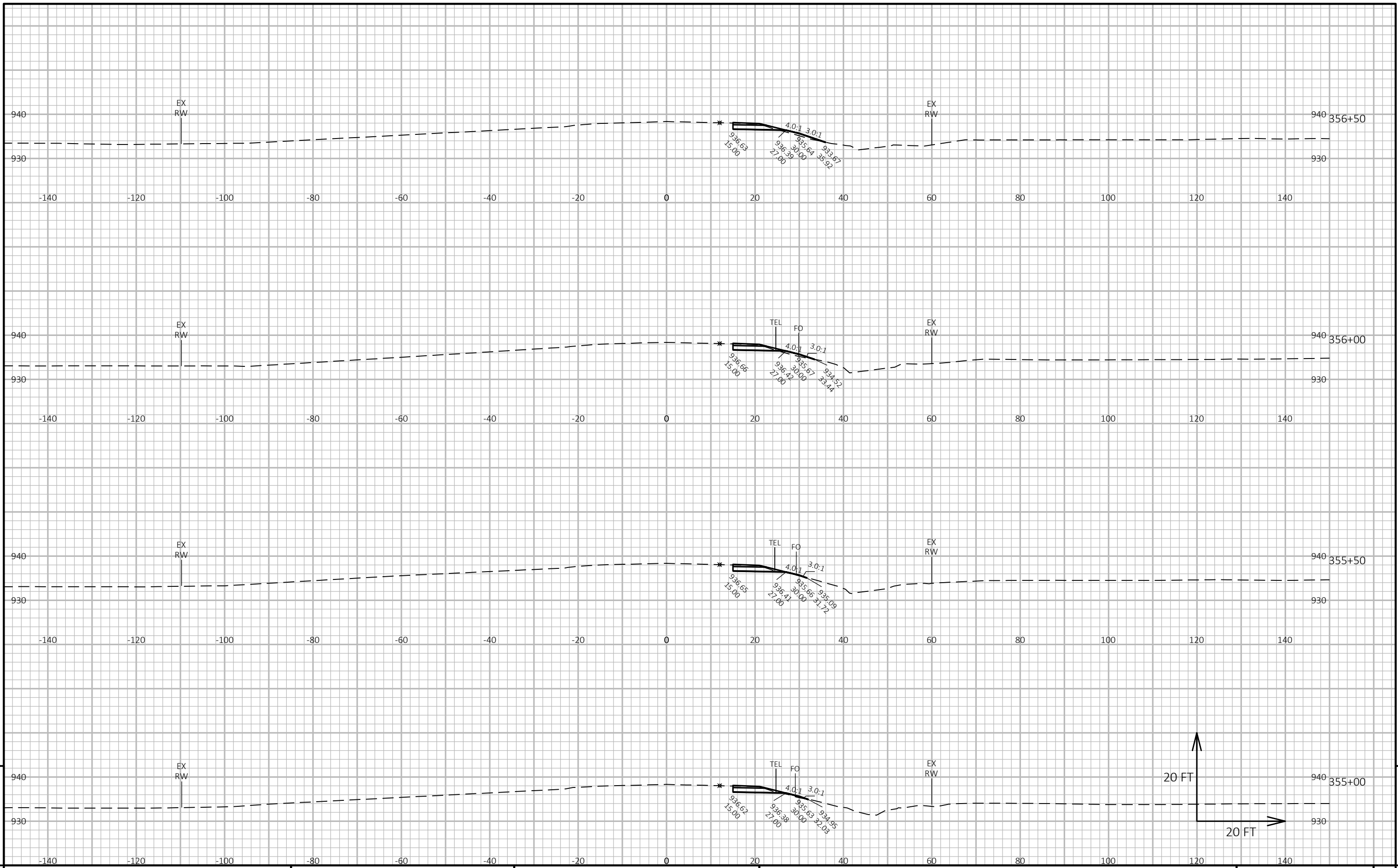
PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET 9

FILE NAME: \\EAURTOP\PLP\I01\N3PUBLIC\PDS\C3D\70800102\SHEETSPLAN\090200-CROSS SECTIONS\090200_XS_USH12.DWG PLOT DATE: 10/31/2019 9:18 AM PLOT BY: PAYNE, MATTHEW A PLOT NAME: PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT. WISDOT/CADD SHEET 49

SURFACE RETURNED TO EXISTING CONDITION
UPON GEOTEXTILE FABRIC REMOVAL
STA 345+09 - STA 353+53



PROJECT NO: 7080-01-72 HWY: USH 12 COUNTY: EAU CLAIRE CROSS SECTIONS: STAGE 3 - USH 12 SHEET E



PROJECT NO: 7080-01-72

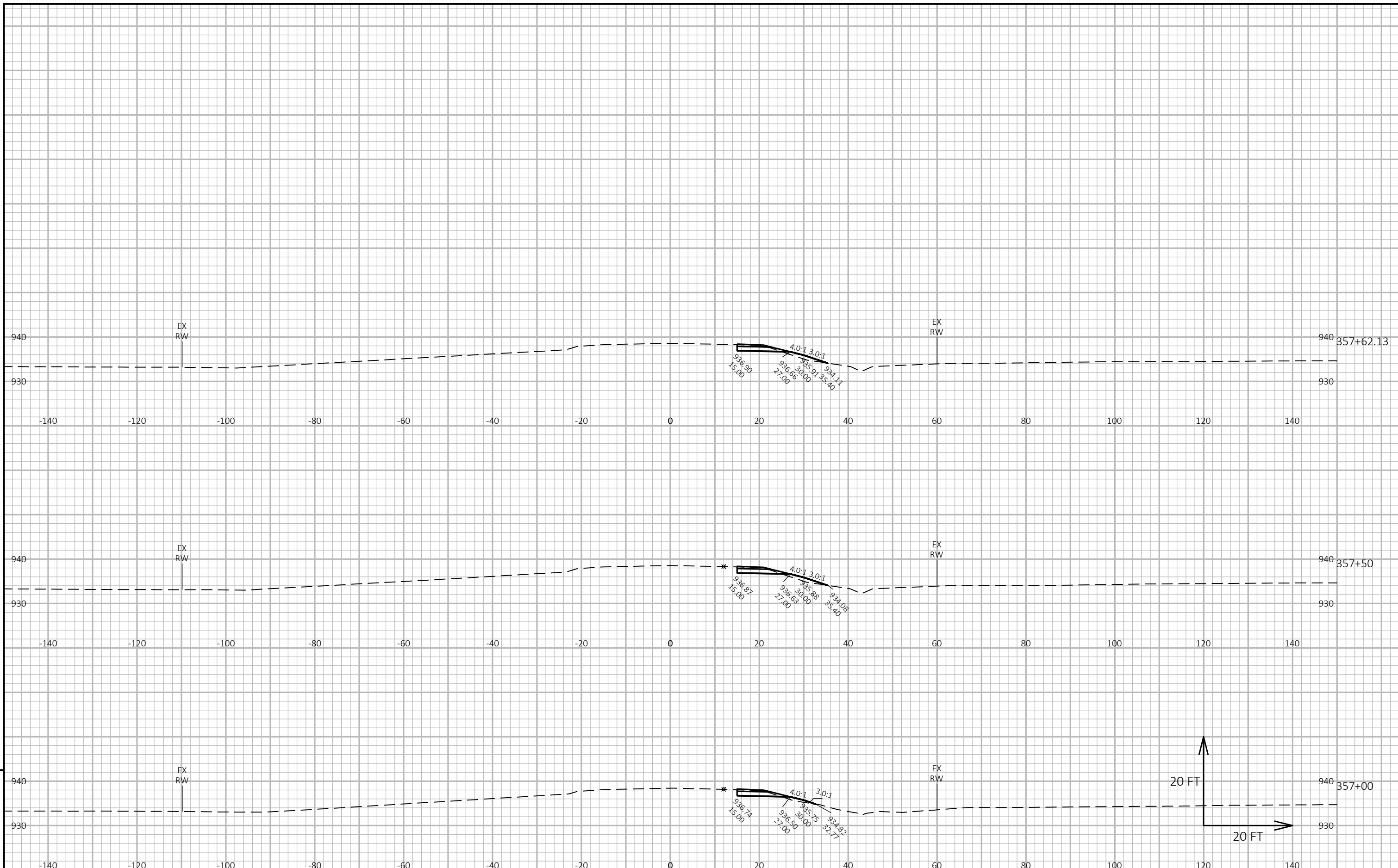
HWY: USH 12

COUNTY: EAU CLAIRE

CROSS SECTIONS: STAGE 3 - USH 12

SHEET

E



PROJECT NO: 7080-01-72

HWY: USH 12

COUNTY: EAU CLAIRE

CROSS SECTIONS: STAGE 3 - USH 12

SHEET

E



Wisconsin Department of Transportation

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