SUP

PROJECT ID: 1570-00-74

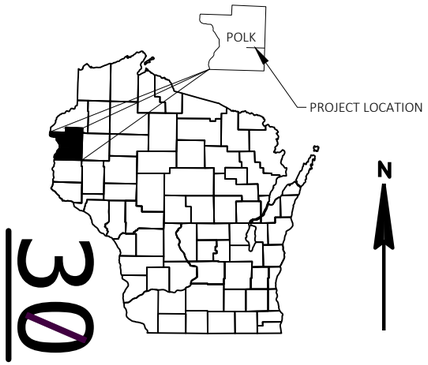
WITH: 1570-00-77

COUNTY: POLK

MAR 10, 2020

ORDER OF SHEETS

TOTAL SHEETS = 146



30

DESIGN DESIGNATION 1570-00-04

A.A.D.T.	2023	=	6200
A.A.D.T.	2043	=	6800
D.H.V.	2043	=	898
D.D.		=	63/37
T.		=	13.8
DESIGN SPEED		=	2,400,000
ESALS		=	60 MPH

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1570-00-74	WISC 2020101	1
1570-00-77	WISC 2020102	1

ST CROIX FALLS - TURTLE LAKE

STH 46 SOUTH JUNCTION TO FRONT AVE

USH 8

POLK

STATE PROJECT NUMBER

1570-00-74

ST CROIX FALLS - TURTLE LAKE

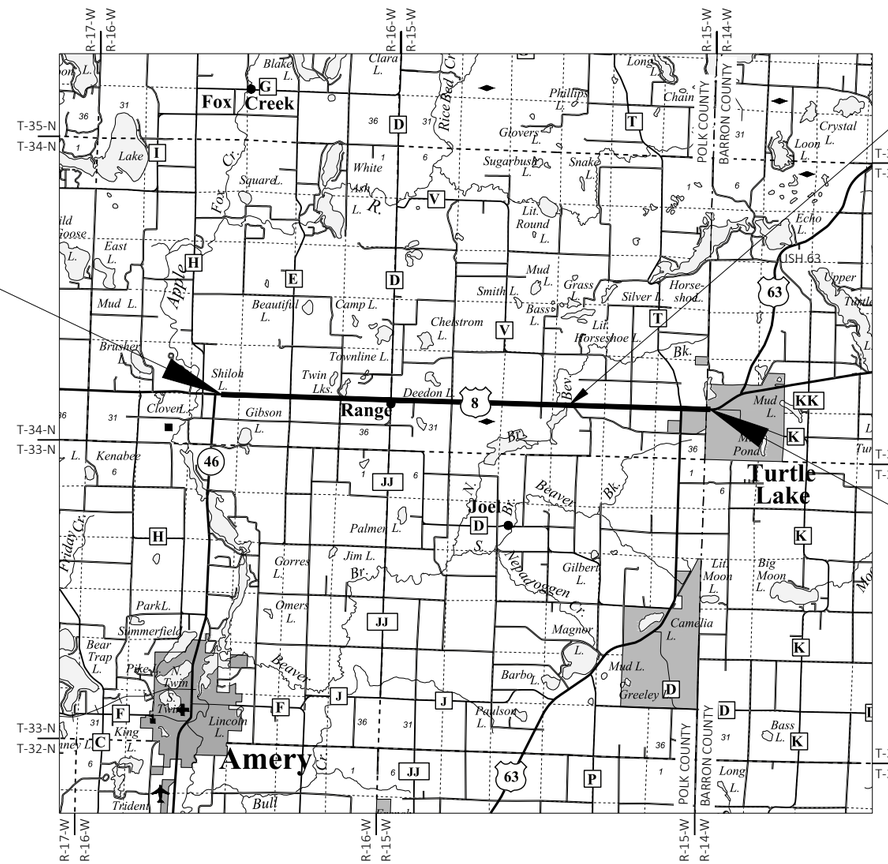
STH 46 SOUTH JUNCTION TO FRONT AVE

USH 8

POLK

STATE PROJECT NUMBER

1570-00-77



NET EXCEPTION TO CL LENGTH
STA 1097+72 - STA 1098+13

BEGIN PROJECT 1570-00-74 & 1570-00-77
STA 736+13.00
X = 536,824.02
Y = 268,005.56

END PROJECT 1570-00-74 & 1570-00-77
STA 1251+31.00
X = 588,333.76
Y = 268,108.30

LAYOUT

SCALE 0 3 MI

TOTAL NET LENGTH OF CENTERLINE = 9.749 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), POLK COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (1991). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 09.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

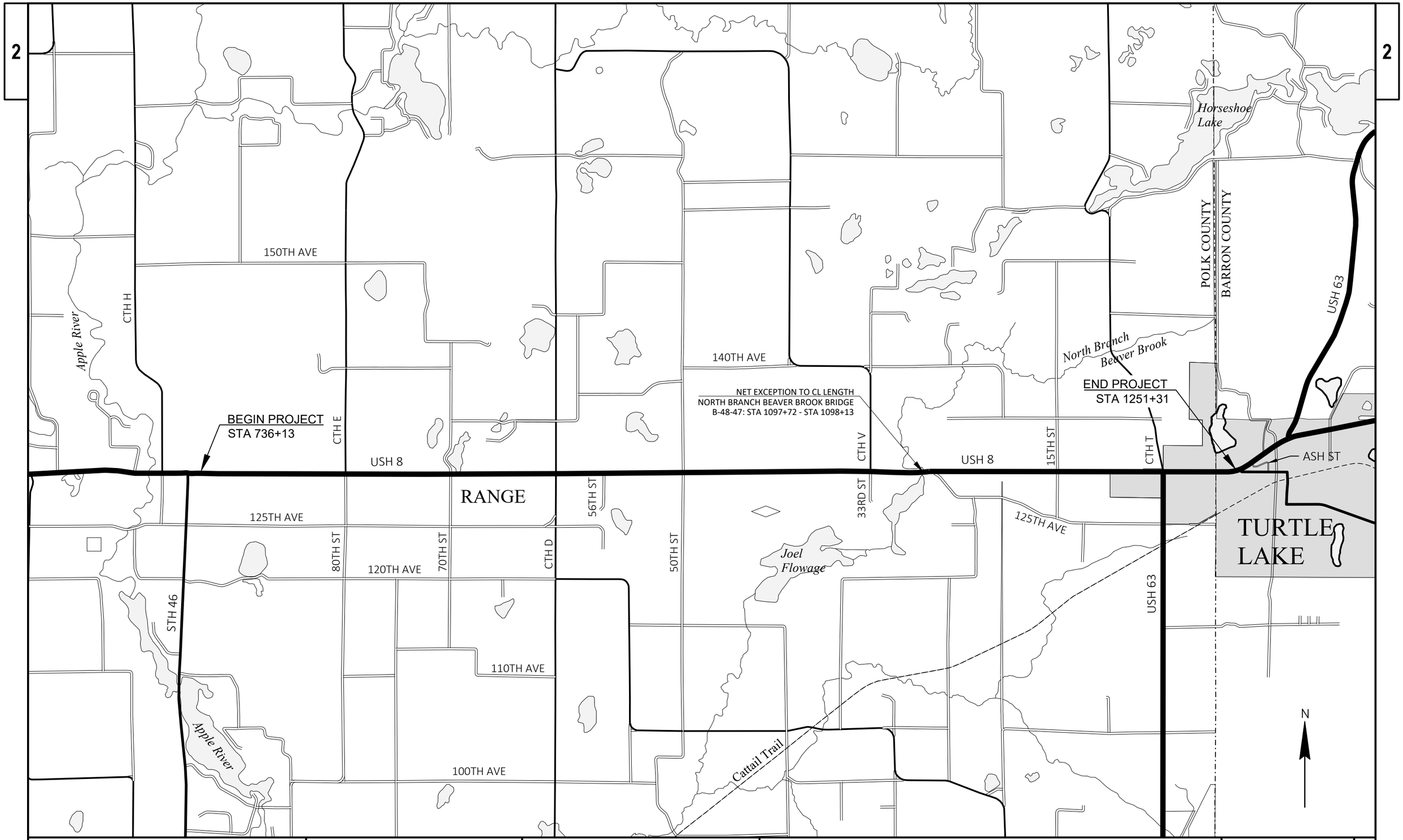
PREPARED BY

Surveyor	WISDOT
Designer	ZACHARY ERICKSON
Project Manager	MATTHEW DICKENSON
Regional Examiner	TOU YANG
Regional Supervisor	JEFFREY OSLON

APPROVED FOR THE DEPARTMENT

DATE: 10/11/2019 Matthew J Dickenson (Signature)

E



PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PROJECT OVERVIEW	SHEET 2 E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PO.DWG
LAYOUT NAME - PO-1

PLOT DATE : 10/8/2019 4:45 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : Custom

WISDOT/CADDS SHEET 42

2

GENERAL NOTES

THE ENGINEER WILL DETERMINE ANY DETAILS OF CONSTRUCTION NOT SHOWN ON THE PLAN.

ALL WASTE MATERIAL RESULTING FROM THE VARIOUS OPERATIONS UNDER THIS CONTRACT SHALL BE COLLECTED TO BE PROPERTY DISPOSED OF PRIOR TO REOPENING LANES TO TRAFFIC.

DISTURBED AREAS WITHIN THE RIGHT OF WAY AS A RESULT OF THIS PROJECT SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

WOOD POST SIZES, FOR TYPE II SIGNING, ARE ESTIMATED LENGTHS AND THE ACTUAL LENGTH WILL BE DETERMINED IN THE FIELD.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS ARE APPROXIMATE AN MAY BE ADJUSTED TO FIT FIELD CONDITIONS.

THE CONTRACTOR SHALL PRESERVE PAVEMENT MARKING TRANSITIONS AND ENSURE NEW PAVEMENT MARKING MATCHES THAT WHICH EXISTED PRIOR TO RESURFACING.

THE LOCATION OF EXISTING AND PROPOSED UTILITY FACILITIES AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

ACCESS TO ALL RESIDENCES SHALL BE MAINTAINED DURING CONSTRUCTION.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE EXACT LOCATIONS OF EROSION CONTROL ITEMS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

CURVE DATA SHOWN IS "ARC DEFINITION".

INSTALL/MAINTAIN ADVANCED WARNING SIGNS BEFORE BEGINNING WORK.

MAINTAIN MINIMUM 11-FT LANES ALONG USH 8 AT ALL TIMES.

HMA PAVEMENT LONGITUDINAL CONSTRUCTIONS JOINTS SHALL NOT BE PLACED IN WHEEL PATHS.

RAMPS SHALL BE PAVED WHEN THE DIFFERENCE BETWEEN EXISTING SURFACE AND MILLED SURFACE IS GREATER THAN 2" AND IS INCIDENTAL TO PAVING.

WHEN THE QUANTITY OF THE ITEM OF BASE AGGREGATE DENSE OR HMA PAVEMENT IS MEASURED BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

HMA PAVEMENT AND ASPHALTIC SURFACE WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

COMMUNICATIONS

WISDOT - NW REGION
ATTN: MATTHEW DICKENSON - DESIGN PROJECT MANAGER
1701 N. 4TH STREET
SUPERIOR, WI 54880
PHONE: (715) 395-3022
EMAIL: matthew.dickenson@dot.wi.gov

WISDOT - NW REGION
ATTN: ZACHARY ERICKSON - DESIGN PROJECT LEADER
1701 N. 4TH STREET
SUPERIOR, WI 54880
PHONE: (715) 392-7996
EMAIL: zachary.erickson@dot.wi.gov

WISDOT - NW REGION
JENNIFER BERG - TRAFFIC ENGINEER
718 W. CLAIREMONT AVENUE
EAU CLAIRE, WI 54701
(715) 836-2853
EMAIL: jenniferl.berg@dot.wi.gov

WDNR - NORTHERN REGION HQ
ATTN: AMY CRONK
810 W. MAPLE STREET
SPOONER, WI 54801
PHONE: (715) 635-4229
EMAIL: amy.cronk@wisconsin.gov

POLK COUNTY HIGHWAY DEPARTMENT
EMIL NORBY - HIGHWAY COMMISSIONER
518 MAIN STREET
BALSAM LAKE, WI 54810
PHONE: (715) 485-8700
FAX: (715) 485-8702

WISCONSIN STATE PATROL SPOONER POST
W7102 GREEN VALLEY ROAD
SPOONER, WI 54801
PHONE: (715) 635-2141
EMERGENCY: (715) 635-7725
FAX: (715) 635-6373

POLK COUNTY SHERIFF'S OFFICE
POLK COUNTY JUSTICE CENTER
1005 WEST MAIN STREET, SUITE 900
BALSAM LAKE, WI 54810
EMERGENCY 911
DISPATCH (715) 485-8300

UTILITIES

COMMUNICATIONS LINE

CENTURYLINK - COMMUNICATION LINE
ATTN: MICHAEL VANDEN BOS
2426 75TH AVENUE
OSCEOLA, WI 54020
PHONE: (715) 294-2463
EMAIL: mike.vandenbos@centurylink.com

CHARTER COMMUNICATIONS - COMMUNICATIONS LINE
ATTN: JAMEY OLDEEN
2304 S MAIN ST
RICE LAKE, WI 54868
PHONE: (715) 719-0561
MOBILE: (715) 651-7488
EMAIL: jamey.oldeen@charter.com

MOSAIC TELECOM - COMMUNICATIONS LINE
ATTN: DENNIS RUSSETT
410 S. 1ST STREET
CAMERON, WI 54822
PHONE: (715) 458-5378
MOBILE: (715) 548-5518
EMAIL: ctcdennis@mosaictelecom.com

NORTHWEST COMMUNITY COMMUNICATIONS INC. - COMMUNICATIONS LINE
ATTN: GREG CARDINAL
116 HARRIMAN AVENUE N.
AMERY, WI 54001
PHONE: (715) 268-3379
MOBILE: (715) 554-1620
EMAIL: gregcardinal@amerytel.com

ELECTRICITY - DISTRIBUTION

POLK-BURNETT ELECTRIC COOPERATIVE - ELECTRICITY
ATTN: ED JOHANSEN
1001 STATE ROAD 35
CENTURIA, WI 54824
PHONE: (715) 646-2191, EXT.322
EMAIL: ejohansen@polkburnett.com

DAIRYLAND POWER COOPERATIVE - ELECTRICITY
ATTN: ROB MALY
3200 EAST AVE S.
P.O. BOX 817
LA CROSSE, WI 54602-0817
PHONE: (608) 518-2633
EMAIL: rob.maly@dairylandpower.com

GAS

WE ENERGIES - GAS
ATTN: STEVEN CHAVERS
104 W. SOUTH STREET
RICE LAKE, WI 54868
PHONE: (715) 234-9605
MOBILE: (715) 213-4327
EMAIL: steven.chavers@we-energies.com

SANITARY AND WATER

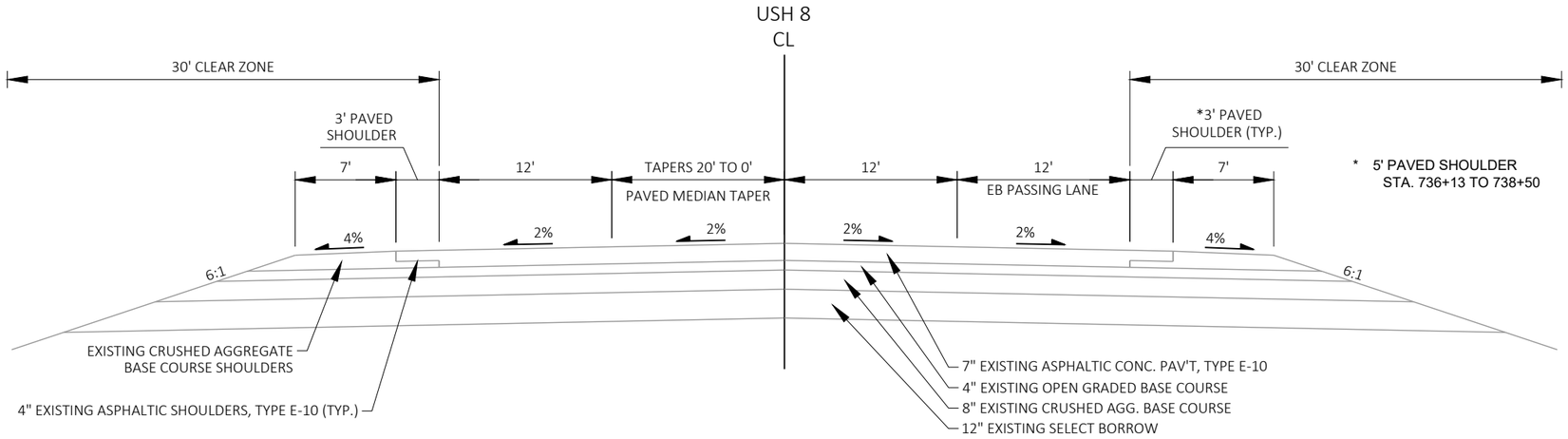
VILLAGE OF TURTLE LAKE - SANITARY & WATER MAIN
ATTN: CORY DAVIS, DIRECTOR OF PUBLIC WORKS
520 LOGAN AVENUE E.
P.O. BOX 11
TURTLE LAKE, WI 54889
PHONE: (715) 986-2820
MOBILE: (715) 641-0582
EMAIL: vtlpw@turtlelake.com

STANDARD ABBREVIATIONS

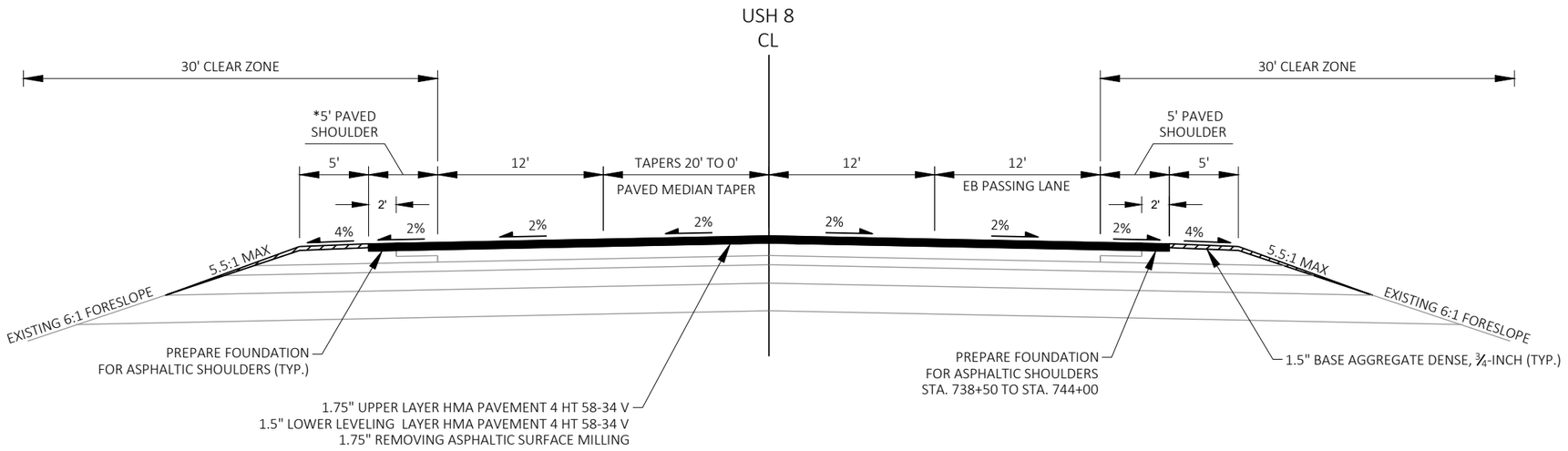
ABUT.	ABUTMENT
AGG.	AGGREGATE
AH.	AHEAD
AADT	ANNUAL AVERAGE DAILY TRAFFIC
APPROX.	APPROXIMATE
AEW	APRON END WALL
ASPH.	ASPHALTIC
BK.	BACK
BEG.	BEGIN
B.M.	BENCH MARK
C/L OR ℄	CENTER LINE
C.E.	COMMERCIAL ENTRANCE
CONC.	CONCRETE
CONSTRUT.	CONSTRUCTION
CO.	COUNTY
CTH	COUNTY TRUNK HIGHWAY
X-SEC.	CROSS SECTION
CR.	CRUSHED
CULV.	CULVERT
DOT	DEPARTMENT OF TRANSPORTATION
D.H.V.	DESIGN HOUR VOLUME
DIA.	DIAMETER
DISCH.	OR DIS. DISCHARGE
E.	EAST
EB	EASTBOUND
EA.	EACH
ELEC.	ELECTRIC
OR ELEV.	ELEVATION
ESALS	EQUIVALENT SINGLE AXLE LOADS
E.B.S.	EXCAVATION BELOW SUBGRADE
EL.	EXISTING
FERT.	FERTILIZE
FE	FIELD ENTRANCE
FIN.	FINISHED
FL OR ℄	FLOW LINE
HOR.	HORIZONTAL
INL.	INLET
INTER.	INTERSECTION
INV.	INVERT
LT	LEFT
L.H.F.	LEFT-HAND FORWARD
LF	LINEAR FOOT
LS	LUMP SUM
MAX.	MAXIMUM
MISC.	MISCELLANEOUS
N.	NORTH
NE	NORTHEAST
NW	NORTHWEST
PAVT.	PAVEMENT
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
PT	POINT OF TANGENCY
POT	POINT ON TANGENT
PE	PRIVATE ENTRANCE
PROJ.	PROJECT
R	RANGE
REQ'D	REQUIRED
R/L	REFERENCE LINE
RT	RIGHT
R.H.F.	RIGHT-HAND FORWARD
R/W	RIGHT-OF-WAY
RD	ROAD
SHLD	SHOULDER
S.	SOUTH
SDD	STANDARD DETAIL DRAWINGS
SR	SIDE ROAD
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
STH	STATE TRUNK HIGHWAYS
STA.	STATION
STRUCT.	STRUCTURE
TEL	TELEPHONE
TEMP.	TEMPORARY
T	TOWN
T.	TRUCKS (PERCENT OF)
TYP.	TYPICAL
UG	UNDERGROUND
VAR.	VARIABLE
V	VELOCITY OR DESIGN SPEED
VC	VERTICAL CURVE
W.	WEST
WB	WESTBOUND
WD	WORKING DAY
WZ	WORK ZONE
X	EAST GRID COORDINATE
Y	NORTH GRID COORDINATE

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

2



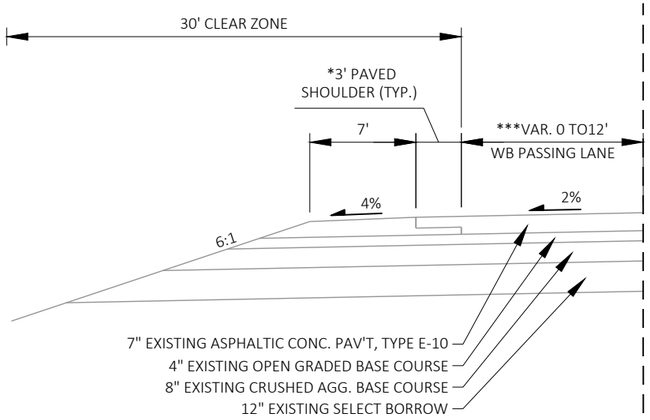
TYPICAL EXISTING SECTION
STA. 736+13 TO 744+00



TYPICAL FINISHED SECTION
STA.736+13 TO 744+00

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN: TYPICAL SECTIONS	SHEET 4	E

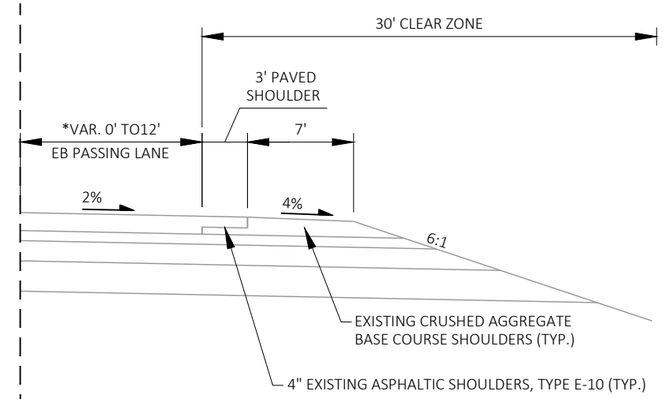
FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_TS.DWG
LAYOUT NAME - TS-1
PLOT DATE : 10/8/2019 4:45 PM
PLOT BY : ERICKSON, ZACHARY A
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42



WESTBOUND PASSING LANES

STA. 815+50 TO 822+50 LT (TAPERS OUT 0' TO 12')
STA. 822+50 TO 884+00 LT (FULL WIDTH 12')
STA. 884+00 TO 891+00 LT (TAPERS IN 12' TO 0')

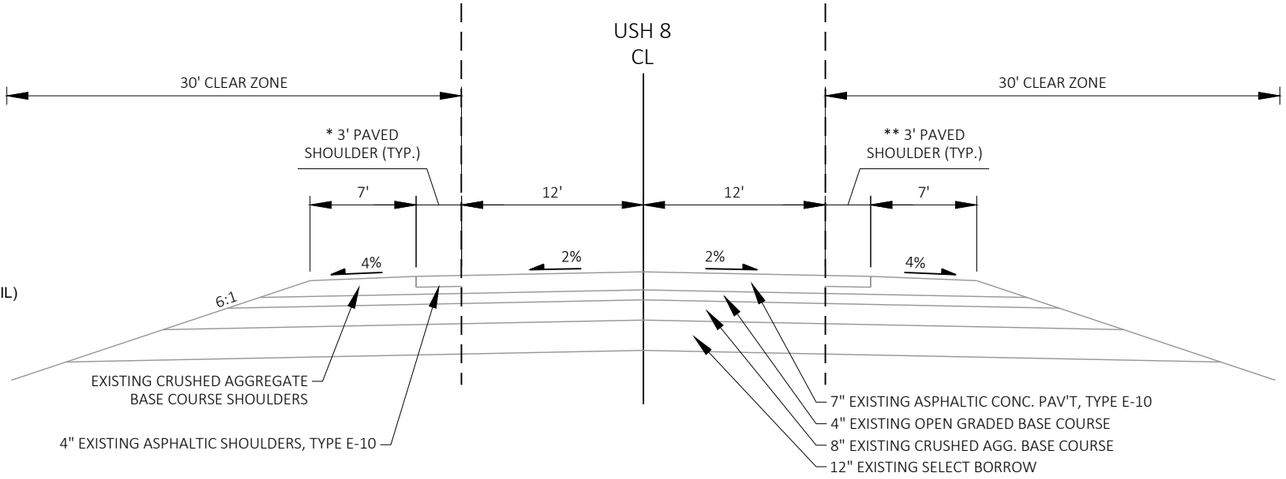
STA. 1112+00 TO 1119+50 LT (TAPERS OUT 0' TO 12')
STA. 1119+50 TO 1185+00 LT (FULL WIDTH 12')
STA. 1185+00 TO 1192+00 LT (TAPERS IN 12' TO 0')



EASTBOUND PASSING LANES

STA. 744+00 TO 794+00 RT (FULL WIDTH 12')
STA. 794+00 TO 801+00 RT (TAPER IN 12' TO 0')

STA. 993+00 TO 1000+00 RT (TAPERS OUT 0' TO 12')
STA. 1000+00 TO 1056+00 RT (FULL WIDTH 12')
STA. 1056+00 TO 1063+00 RT (TAPER IN 12' TO 0')



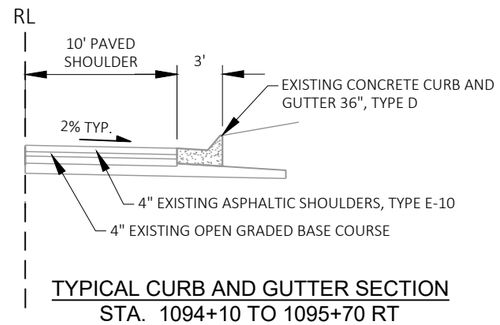
* WESTBOUND RIGHT TURN LANES (SEE TURN LANE DETAIL)
STA. 807+10 TO 828+20 LT
STA. 859+60 TO 865+60 LT
STA. 907+25 TO 907+80 LT (TAPER ONLY)
STA. 915+50 TO 916+95 LT (TAPER ONLY)
STA. 923+70 TO 927+85 LT
STA. 975+80 TO 981+60 LT
STA. 1069+60 TO 1075+20 LT
STA. 1163+25 TO 1168+65 LT

PAVED SHOULDER WIDENS AT APPROACH TO GUARDRAIL
STA. 1096+05 TO 1096+72 LT (5' TO 12' TAPER OUT TO EAT)
STA. 1099+13 TO 1099+80 LT (12' TO 5' TAPER IN FROM EAT)

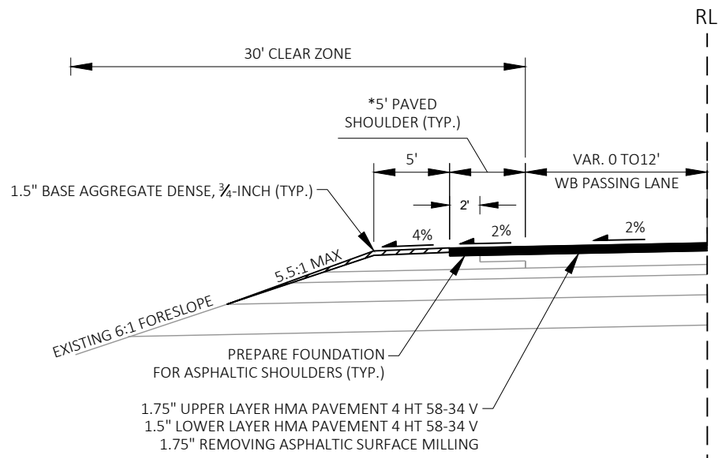
TYPICAL EXISTING SECTION
STA. 744+00 TO 907+80
STA. 915+50 TO 1096+72
STA. 1099+13 TO 1206+28

** EASTBOUND RIGHT TURN LANES (SEE TURN LANE DETAIL)
STA. 804+40 TO 814+25 RT
STA. 821+20 TO 828+20 RT
STA. 855+50 TO 862+50 RT
STA. 906+10 TO 907+80 RT (TAPER ONLY)
STA. 935+95 TO 939+15 RT
STA. 973+20 TO 978+80 RT
STA. 1067+40 TO 1074+25 RT
STA. 1099+85 TO 1104+60 RT
STA. 1160+10 TO 1165+95 RT

PAVED SHOULDER WIDENS AT APPROACH TO GUARDRAIL
STA. 1092+97 TO 1094+10 RT (5' TO 10' TAPER OUT)
STA. 1094+10 TO 1095+70 RT (10' PAVED WITH 36" C&G)
STA. 1095+70 TO 1096+72 RT (10' TO 12' TAPER OUT TO EAT)
STA. 1099+13 TO 1099+80 RT (12' TO 10' TAPER IN FROM EAT)



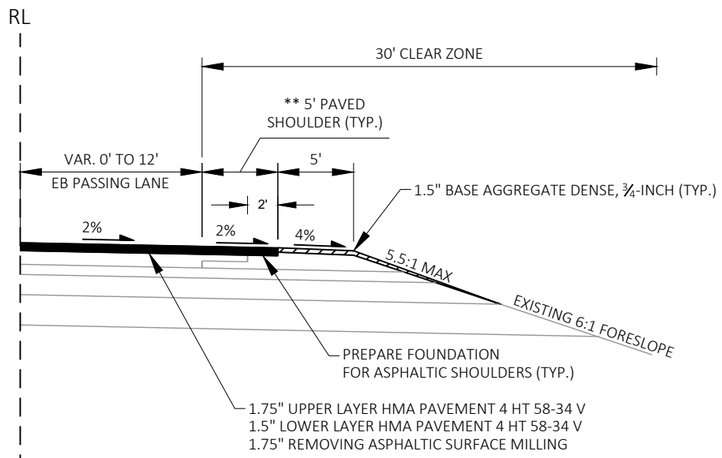
TYPICAL CURB AND GUTTER SECTION
STA. 1094+10 TO 1095+70 RT



WESTBOUND PASSING LANES

STA. 815+50 TO 822+50 LT (TAPERS OUT 0' TO 12')
STA. 822+50 TO 884+00 LT (FULL WIDTH 12')
STA. 884+00 TO 891+00 LT (TAPERS IN 12' TO 0')

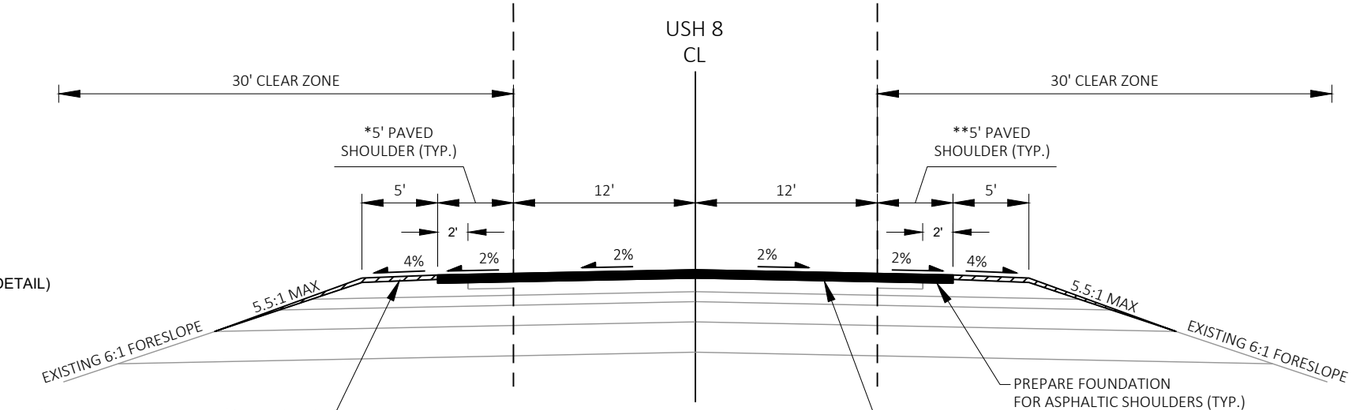
STA. 1112+00 TO 1119+50 LT (TAPERS OUT 0' TO 12')
STA. 1119+50 TO 1185+00 LT (FULL WIDTH 12')
STA. 1185+00 TO 1192+00 LT (TAPERS IN 12' TO 0')



EASTBOUND PASSING LANES

STA. 744+00 TO 794+00 RT (FULL WIDTH 12')
STA. 794+00 TO 801+00 RT (TAPER IN 12' TO 0')

STA. 993+00 TO 1000+00 RT (TAPERS OUT 0' TO 12')
STA. 1000+00 TO 1056+00 RT (FULL WIDTH 12')
STA. 1056+00 TO 1063+00 RT (TAPER IN 12' TO 0')



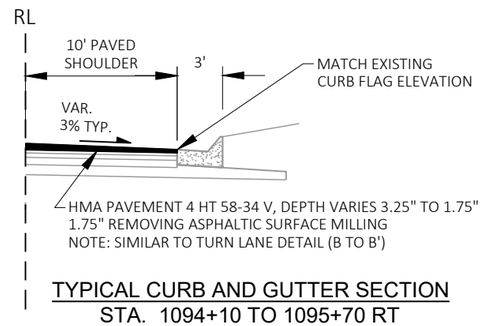
TYPICAL FINISHED SECTION
STA. 744+00 TO 907+80
STA. 915+50 TO 1096+72
STA. 1099+13 TO 1206+28

* WESTBOUND RIGHT TURN LANES (SEE TURN LANE DETAIL)
STA. 807+10 TO 828+20 LT
STA. 859+60 TO 865+60 LT
STA. 907+25 TO 907+80 LT (TAPER ONLY)
STA. 915+50 TO 916+95 LT (TAPER ONLY)
STA. 923+70 TO 927+85 LT
STA. 975+80 TO 981+60 LT
STA. 1069+60 TO 1075+20 LT
STA. 1163+25 TO 1168+65 LT

PAVED SHOULDER WIDENS AT APPROACH TO GUARDRAIL
STA. 1096+05 TO 1096+72 LT (5' TO 12' TAPER OUT TO EAT)
STA. 1099+13 TO 1099+80 LT (12' TO 5' TAPER IN FROM EAT)

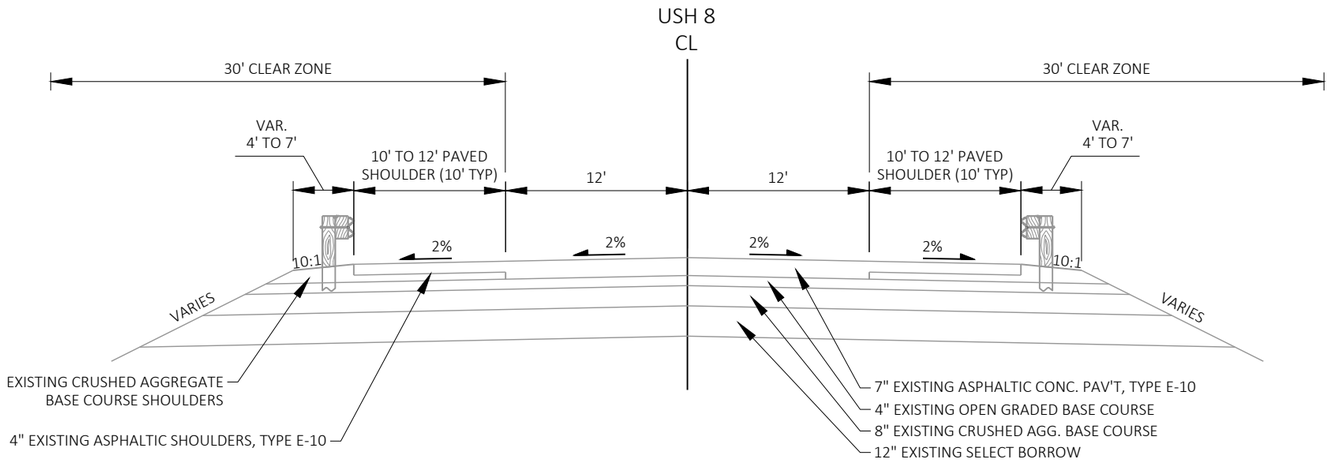
** EASTBOUND RIGHT TURN LANES (SEE TURN LANE DETAIL)
STA. 804+40 TO 814+25 RT
STA. 821+20 TO 828+20 RT
STA. 855+50 TO 862+50 RT
STA. 906+10 TO 907+80 RT (TAPER ONLY)
STA. 935+95 TO 939+15 RT
STA. 973+20 TO 978+80 RT
STA. 1067+40 TO 1074+25 RT
STA. 1099+85 TO 1104+60 RT
STA. 1160+10 TO 1165+95 RT

PAVED SHOULDER WIDENS AT APPROACH TO GUARDRAIL
STA. 1092+97 TO 1094+10 RT (5' TO 10' TAPER OUT)
STA. 1094+10 TO 1095+70 RT (10' PAVED WITH 36" C&G)
STA. 1095+70 TO 1096+72 RT (10' TO 12' TAPER OUT TO EAT)
STA. 1099+13 TO 1099+80 RT (12' TO 10' TAPER IN FROM EAT)

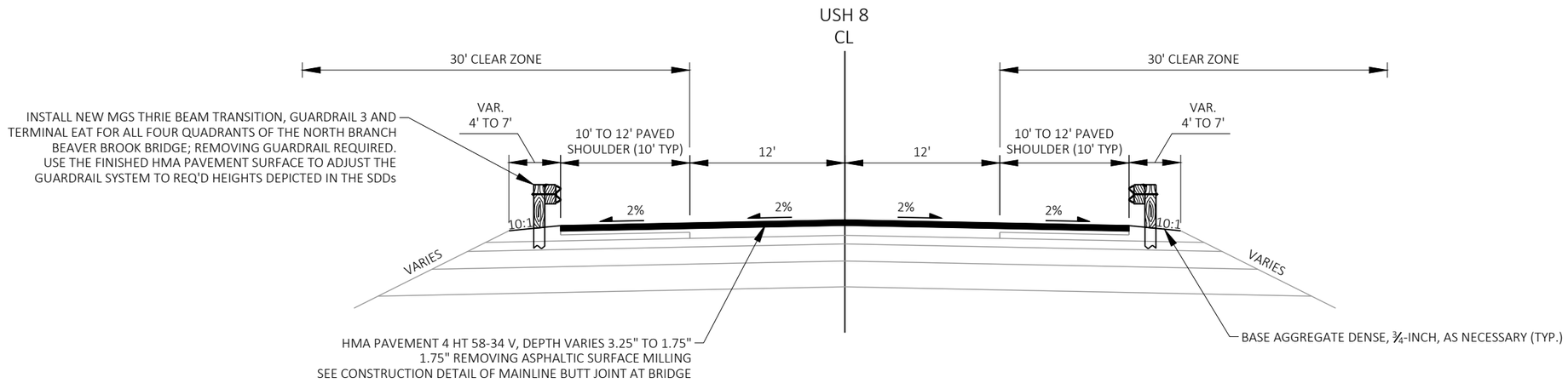


TYPICAL CURB AND GUTTER SECTION
STA. 1094+10 TO 1095+70 RT

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN: TYPICAL SECTIONS	SHEET 6	E

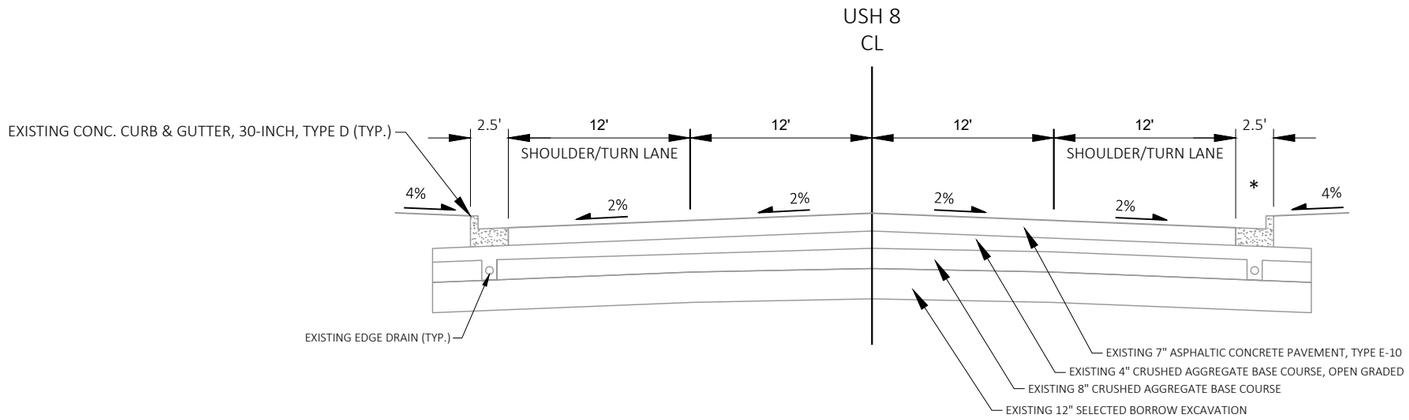


TYPICAL EXISTING SECTION
STA. 1096+72 TO 1097+72
STA. 1098+13 TO 1099+13



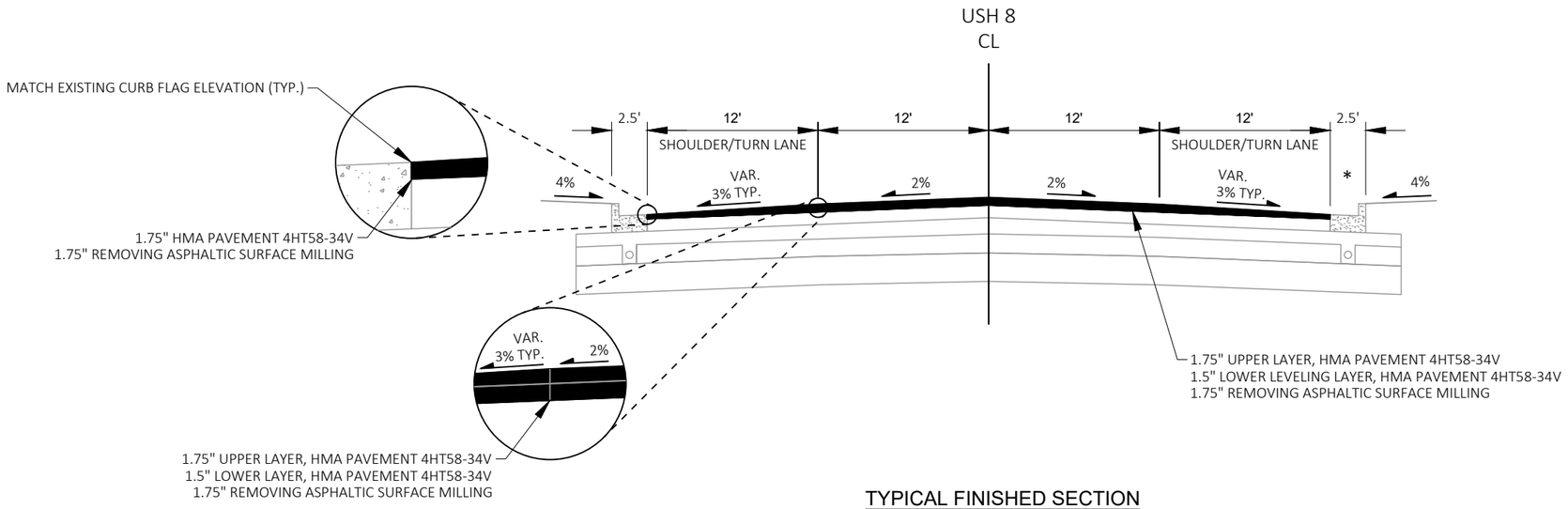
TYPICAL FINISHED SECTION
STA. 1096+72 TO 1097+72
STA. 1098+13 TO 1099+13

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN: TYPICAL SECTIONS	SHEET 7	E



TYPICAL EXISTING SECTION
COMMUNITY OF RANGE
STA. 907+80 TO 915+50

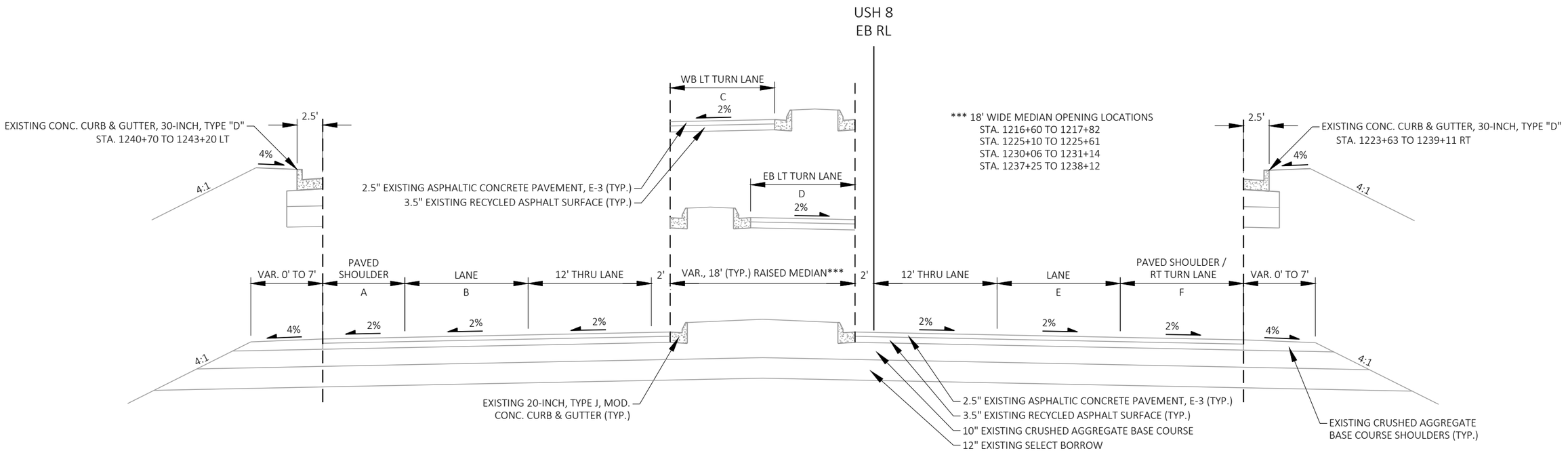
* CURB AND GUTTER ON RIGHT ENDS AT STA. 914+25 RT,
TURN LANE TAPERS IN 12' TO 0' FROM STA. 914+25 TO 915+20 RT. (SEE TURN LANE DETAIL)



TYPICAL FINISHED SECTION
COMMUNITY OF RANGE
STA. 907+80 TO 915+50

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN: TYPICAL SECTIONS	SHEET 8	E

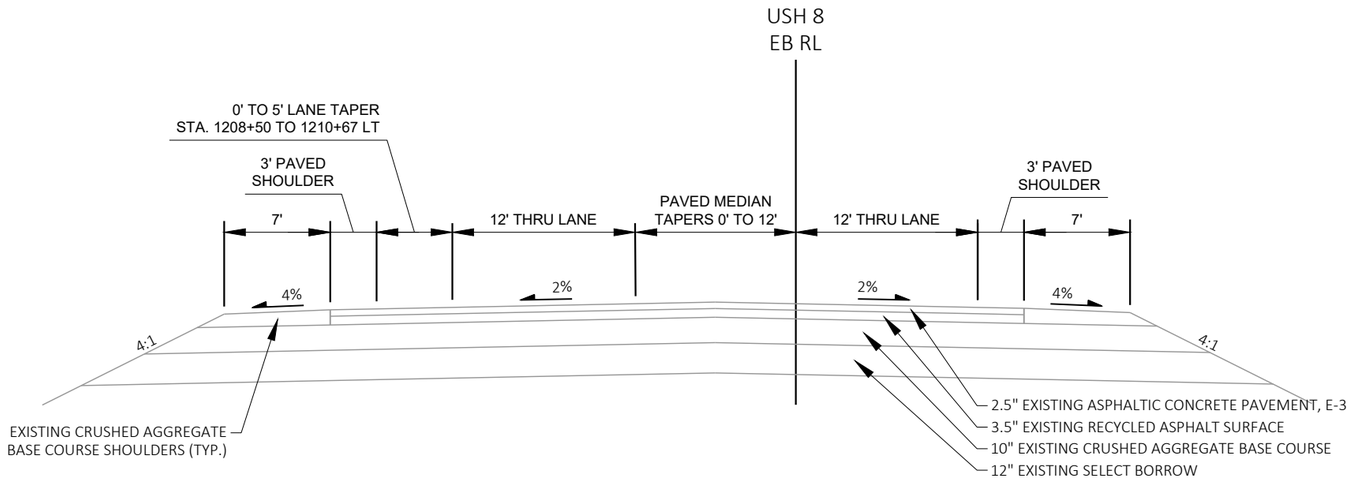
FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_TS.DWG
LAYOUT NAME - TS-4
PLOT DATE : 10/8/2019 4:45 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : 1 IN:10 FT



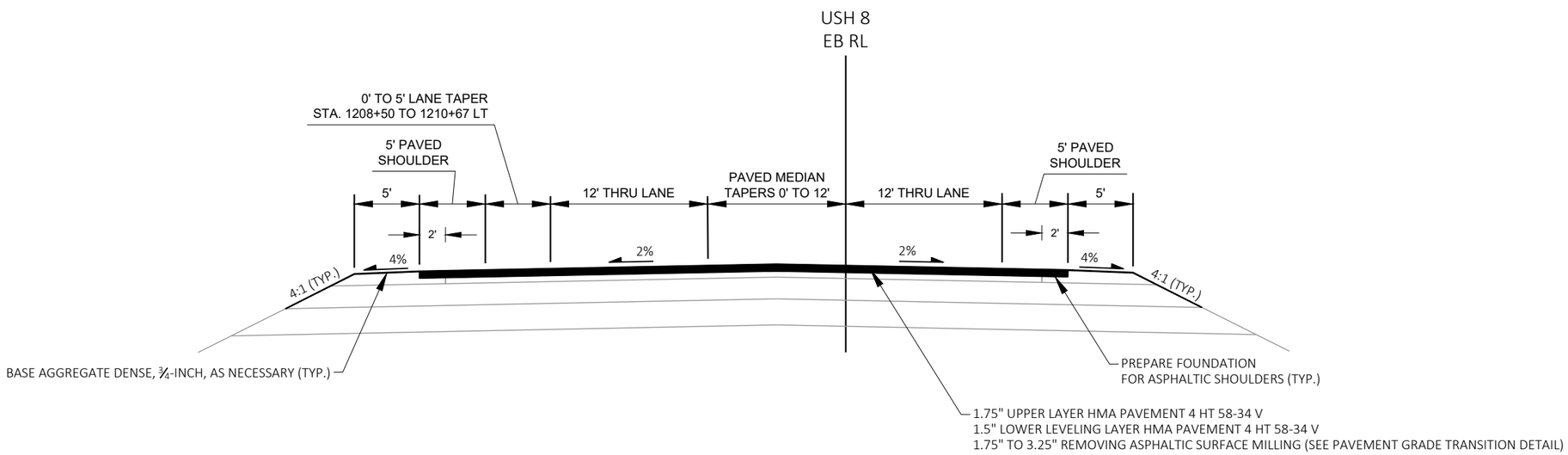
TYPICAL EXISTING SECTION
STA. 1210+67 TO 1243+20

A		B		C		D		E		F	
STA. 1210+67 TO 1215+83	3'	STA. 1210+67 TO 1214+70	5' TO 12'	STA. 1217+82 TO 1222+41	10'	STA. 1210+67 TO 1213+71	0'	STA. 1210+67 TO 1223+63	0'	STA. 1210+67 TO 1211+27	3'
STA. 1215+83 TO 1216+62	3' TO 10'	STA. 1214+70 TO 1243+20	12'	STA. 1222+41 TO 1223+40	10' TO 0'	STA. 1213+71 TO 1214+68	0' TO 10'	STA. 1223+63 TO 1243+20	12'	STA. 1211+27 TO 1213+10	3' TO 12'
STA. 1216+62 TO 1218+01	10'			STA. 1223+40 TO 1231+14	0'	STA. 1214+68 TO 1216+60	10'			STA. 1213+10 TO 1220+00	12'
STA. 1218+01 TO 1240+70	8'			STA. 1231+14 TO 1232+23	10'	STA. 1216+60 TO 1243+20	0'			STA. 1220+00 TO 1223+63	34' TO 21'
STA. 1240+70 TO 1243+20	0'			STA. 1232+23 TO 1233+41	10' TO 0'					STA. 1223+63 TO 1239+11	12'
				STA. 1233+41 TO 1238+12	0'					STA. 1239+11 TO 1243+20	10'
				STA. 1238+12 TO 1239+16	10'						
				STA. 1239+16 TO 1239+65	10' TO 0'						
				STA. 1239+65 TO 1243+20	0'						

2



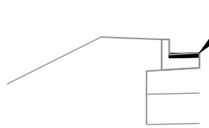
TYPICAL EXISTING SECTION
STA. 1206+28 TO 1210+67



TYPICAL FINISHED SECTION
STA. 1206+28 TO 1210+67

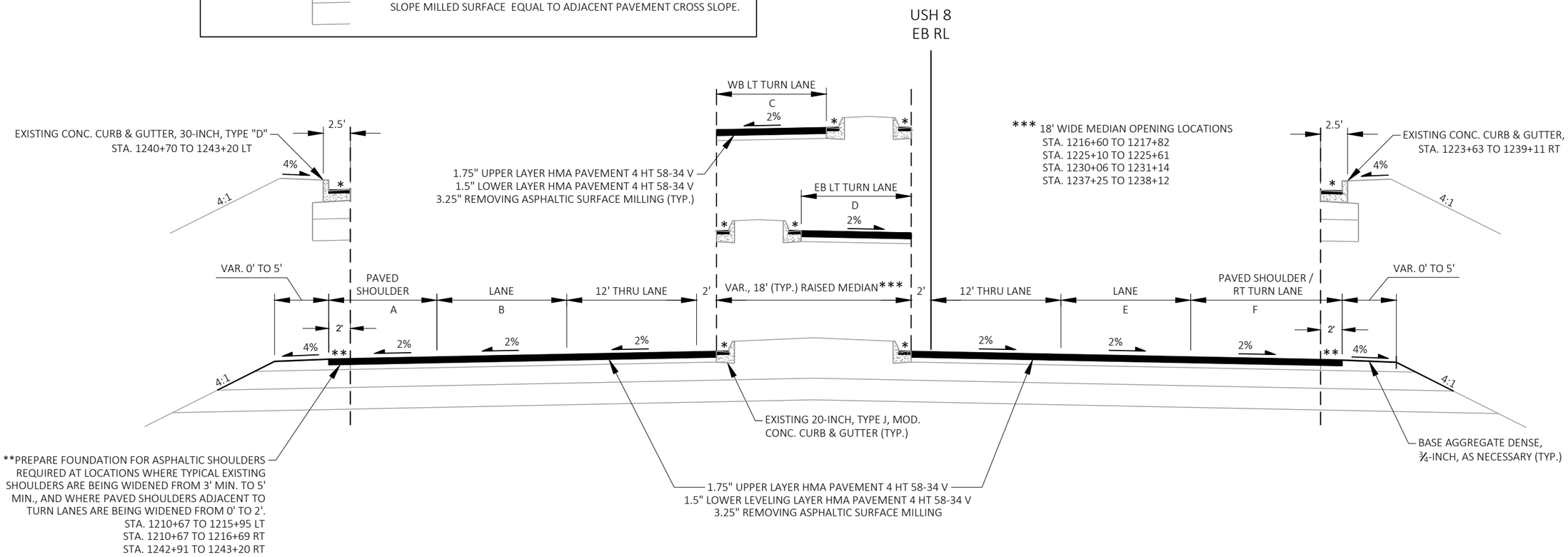
2

* AS DIRECTED BY THE ENGINEER IN THE FIELD, OVERLAY DETERIORATED SEGMENTS OF EXISTING GUTTER PAN. SEE MISCELLANEOUS QUANTITIES FOR ANTICIPATED LOCATIONS.



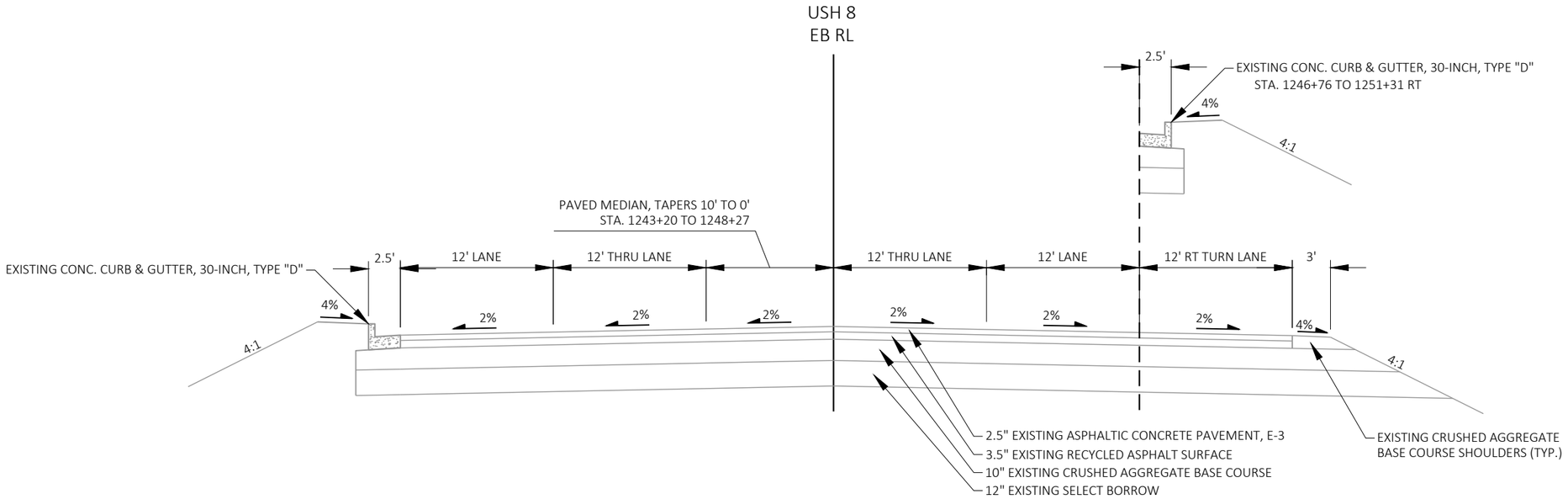
MAINTAIN CURB FLAG ELEVATION (TYP.)
1.75" UPPER LAYER, HMA PAVEMENT 4HT58-34V
1.75" REMOVING ASPHALTIC SURFACE MILLING

NOTE: 1.75" MILL DEPTH MEASURED AT CURB FLAG,
SLOPE MILLED SURFACE EQUAL TO ADJACENT PAVEMENT CROSS SLOPE.



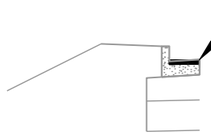
TYPICAL FINISHED SECTION
STA. 1210+67 TO 1243+20

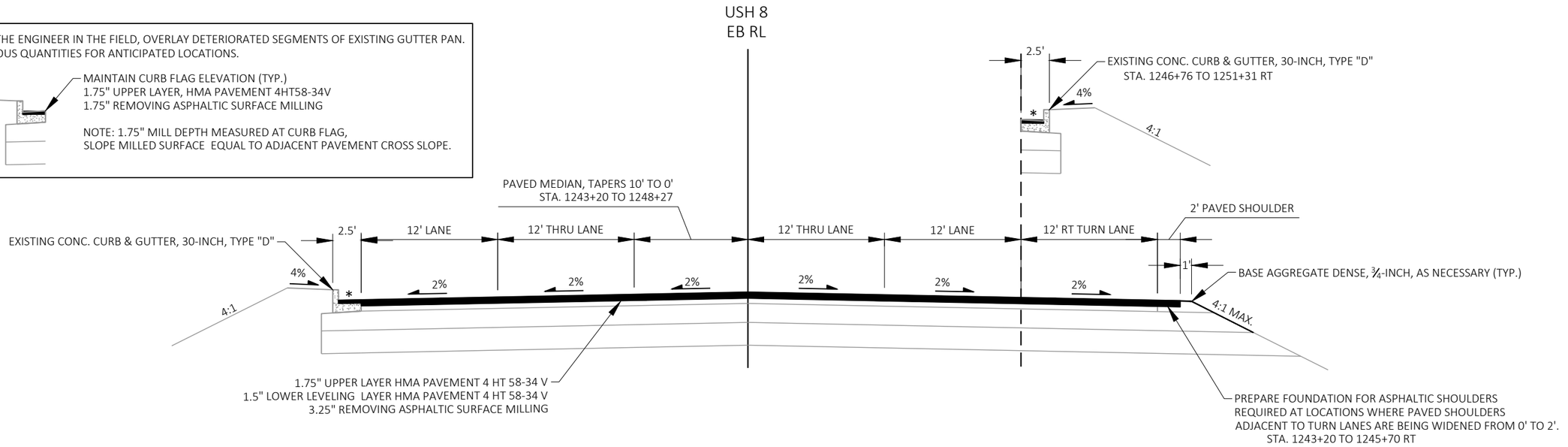
A		B		C		D		E		F	
**STA. 1210+67 TO 1216+18	5'	STA. 1210+67 TO 1214+70	5' TO 12'	STA. 1217+82 TO 1222+41	10'	STA. 1210+67 TO 1213+71	0'	STA. 1210+67 TO 1223+63	0'	**STA. 1210+67 TO 1211+27	5'
**STA. 1216+18 TO 1216+62	5' TO 12'	STA. 1214+70 TO 1243+20	12'	STA. 1222+41 TO 1223+40	10' TO 0'	STA. 1213+71 TO 1214+68	0' TO 10'	STA. 1223+63 TO 1243+20	12'	**STA. 1211+27 TO 1213+10	5' TO 14'
STA. 1216+62 TO 1218+01	12'			STA. 1223+40 TO 1231+14	0'	STA. 1214+68 TO 1216+60	10'			**STA. 1213+10 TO 1216+69	14'
STA. 1218+01 TO 1240+70	8'			STA. 1231+14 TO 1232+23	10'	STA. 1216+60 TO 1243+20	0'			STA. 1216+69 TO 1220+00	12'
STA. 1240+70 TO 1243+20	0'			STA. 1232+23 TO 1233+41	10' TO 0'					STA. 1220+00 TO 1223+63	34' TO 21'
				STA. 1233+41 TO 1238+12	0'					STA. 1223+63 TO 1239+11	12'
				STA. 1238+12 TO 1239+16	10'					STA. 1239+11 TO 1243+20	10'
				STA. 1239+16 TO 1239+65	10' TO 0'						
				STA. 1239+65 TO 1243+20	0'						



TYPICAL EXISTING SECTION
STA. 1243+20 TO 1251+31

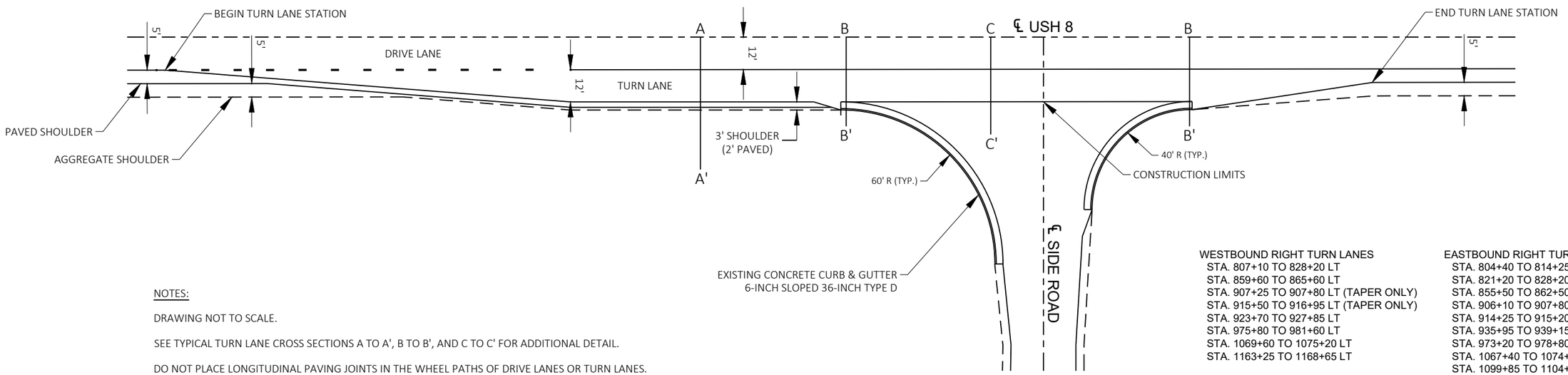
* AS DIRECTED BY THE ENGINEER IN THE FIELD, OVERLAY DETERIORATED SEGMENTS OF EXISTING GUTTER PAN. SEE MISCELLANEOUS QUANTITIES FOR ANTICIPATED LOCATIONS.





TYPICAL FINISHED SECTION
STA. 1243+20 TO 1251+31

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN: TYPICAL SECTIONS	SHEET 12	E



NOTES:

DRAWING NOT TO SCALE.

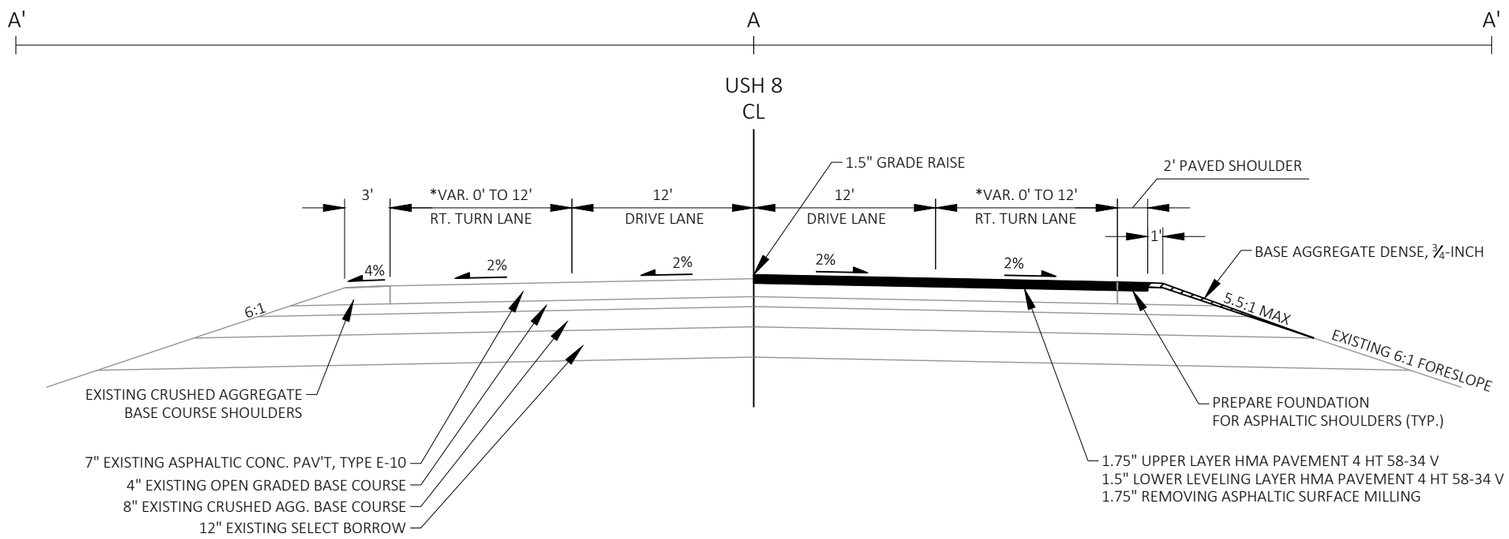
SEE TYPICAL TURN LANE CROSS SECTIONS A TO A', B TO B', AND C TO C' FOR ADDITIONAL DETAIL.

DO NOT PLACE LONGITUDINAL PAVING JOINTS IN THE WHEEL PATHS OF DRIVE LANES OR TURN LANES.

WESTBOUND RIGHT TURN LANES
- STA. 807+10 TO 828+20 LT
- STA. 859+60 TO 865+60 LT
- STA. 907+25 TO 907+80 LT (TAPER ONLY)
- STA. 915+50 TO 916+95 LT (TAPER ONLY)
- STA. 923+70 TO 927+85 LT
- STA. 975+80 TO 981+60 LT
- STA. 1069+60 TO 1075+20 LT
- STA. 1163+25 TO 1168+65 LT

EASTBOUND RIGHT TURN LANES
- STA. 804+40 TO 814+25 RT
- STA. 821+20 TO 828+20 RT
- STA. 855+50 TO 862+50 RT
- STA. 906+10 TO 907+80 RT (TAPER ONLY)
- STA. 914+25 TO 915+20 RT (TAPER ONLY)
- STA. 935+95 TO 939+15 RT
- STA. 973+20 TO 978+80 RT
- STA. 1067+40 TO 1074+25 RT
- STA. 1099+85 TO 1104+60 RT
- STA. 1160+10 TO 1165+95 RT

TURN LANE DETAIL (PLAN VIEW)



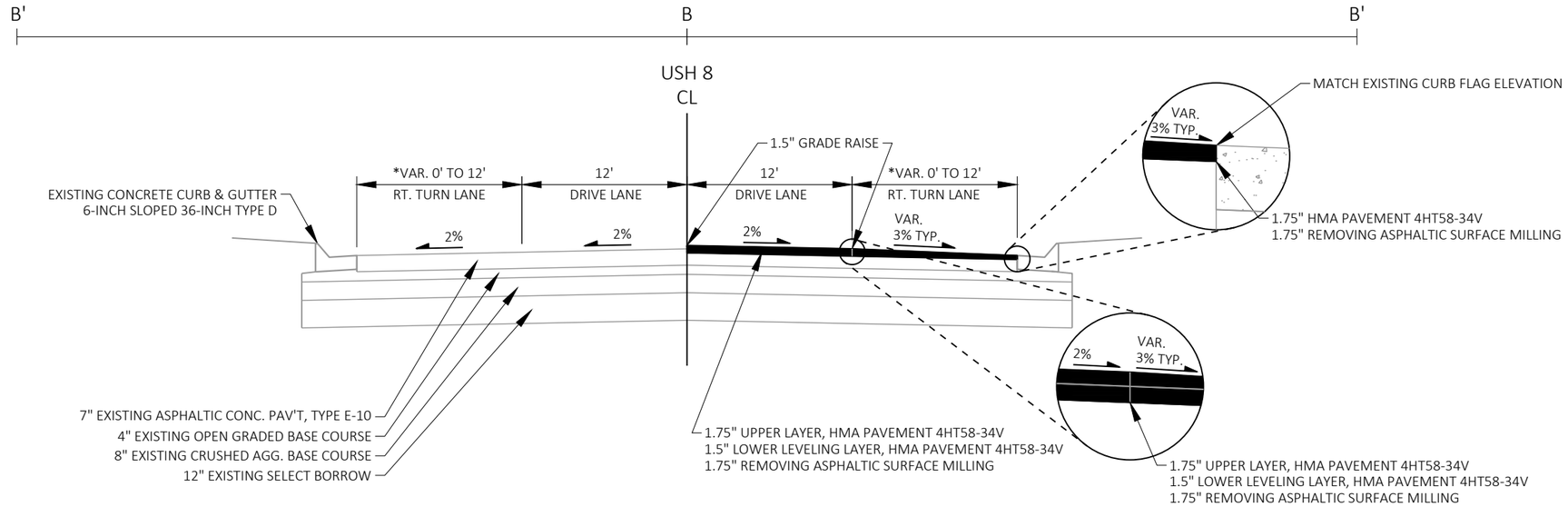
TYPICAL EXISTING HALF SECTION

TYPICAL FINISHED HALF SECTION

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN: TYPICAL SECTIONS	SHEET 13	E

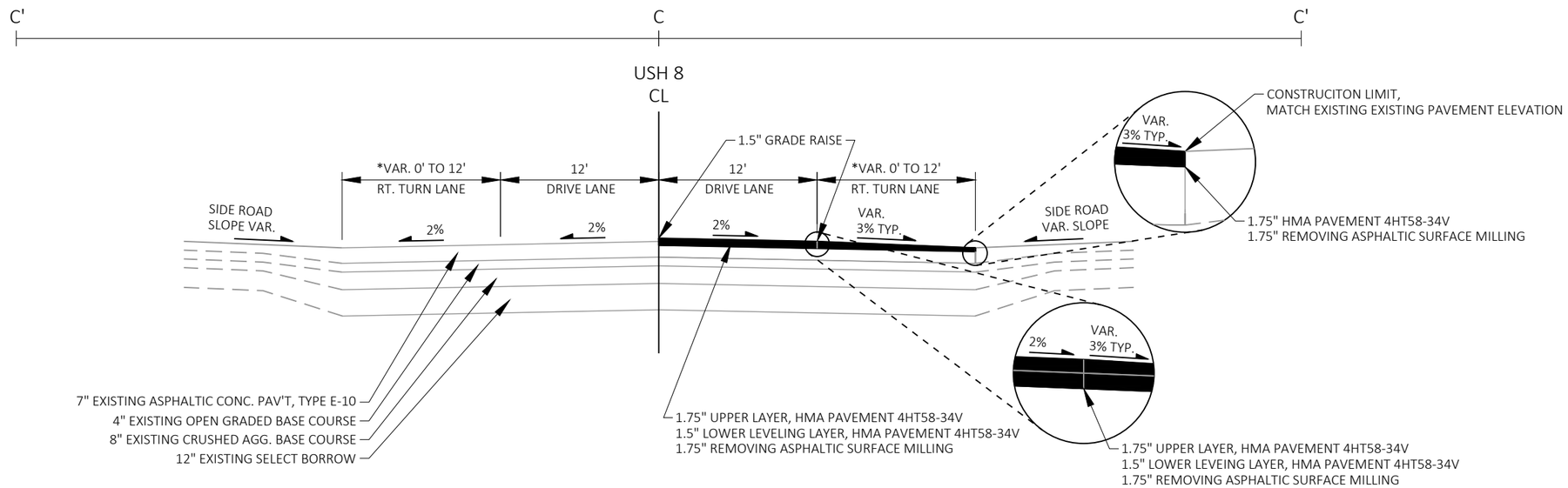
FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_TS.DWG
LAYOUT NAME - TS-9

PLOT DATE : 10/8/2019 4:45 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : 1 IN:40 FT



TYPICAL EXISTING HALF SECTION TYPICAL FINISHED HALF SECTION

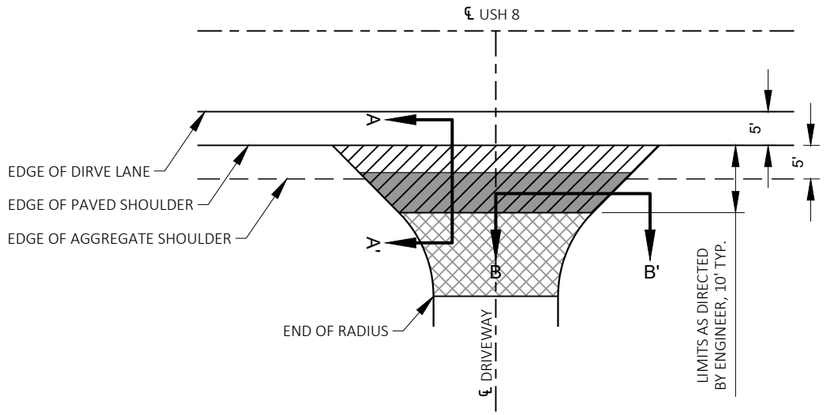
TURN LANE DETAIL (B TO B')



TYPICAL EXISTING HALF SECTION TYPICAL FINISHED HALF SECTION

TURN LANE DETAIL (C TO C')

SUPER ELEVATION TABLE - USH 8			
CURVE START STA.	CURVE END STA.	EXISTING SE	PROPOSED SE
739+50.78	745+60.87	NC	NC
750+08.47	752+81.30	NC	NC
812+99.80	817+72.98	NC	NC
817+72.98	822+10.07	NC	NC
837+37.17	845+68.86	NC	NC
883+64.26	893+20.70	NC	NC
893+20.70	902+76.55	NC	NC
943+30.32	945+21.33	NC	NC
971+97.83	975+91.50	NC	NC
990+66.65	996+26.93	NC	NC
1021+01.01	1023+41.20	NC	NC
1036+11.95	1044+11.42	NC	NC
1044+11.42	1052+00.46	NC	NC
1063+25.81	1069+96.82	NC	NC
1090+06.47	1094+16.16	4.6%	4.6%
1099+00.06	1111+86.74	NC	NC
1111+86.74	1121+64.53	NC	NC
1134+24.02	1136+38.80	NC	NC
1158+60.35	1165+37.38	NC	NC
1178+97.54	1180+99.61	NC	NC
1185+83.20	1195+40.81	NC	NC
1195+40.81	1204+98.06	NC	NC
1248+26.80	1251+21.28	4.0%	4.0%



RESTORE RURAL PAVED DRIVEWAY DETAIL

736+50 RT	790+00 RT	844+54 LT	892+30 LT	903+20 RT
904+20 LT	917+15 LT	928+05 RT	950+50 LT	965+74 LT
1041+32 LT	1050+90 RT	1065+55 RT	1182+65 LT	

LEGEND

= ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES (DEPTH VARIES, 2" TYP., 1.5" MIN.)

= REMOVING ASPHALTIC SURFACE BUTT JOINTS

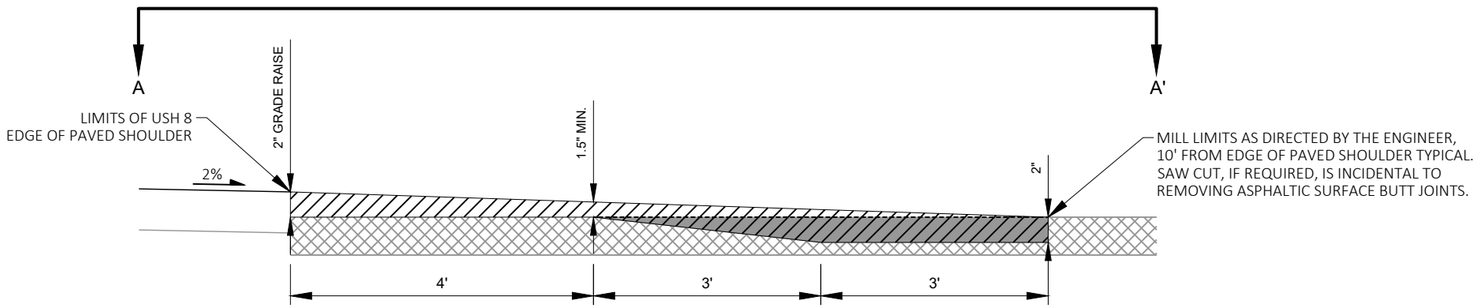
= EXISTING ASPHALTIC DRIVEWAY

NOTES

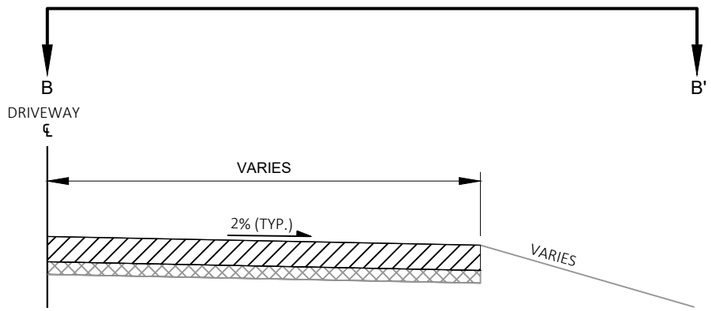
NOT TO SCALE.

MATCH EXISTING PAVED SURFACE WIDTH, RADII AND TAPERS.

ANY ADDITIONAL BASE AGG. DENSE REQUIRED SHALL BE PAID UNDER ITEM 'BASE AGGREGATE DENSE 3/4-INCH'.



DETAIL OF RURAL PAVED DRIVEWAY BUTT JOINT (A-A')



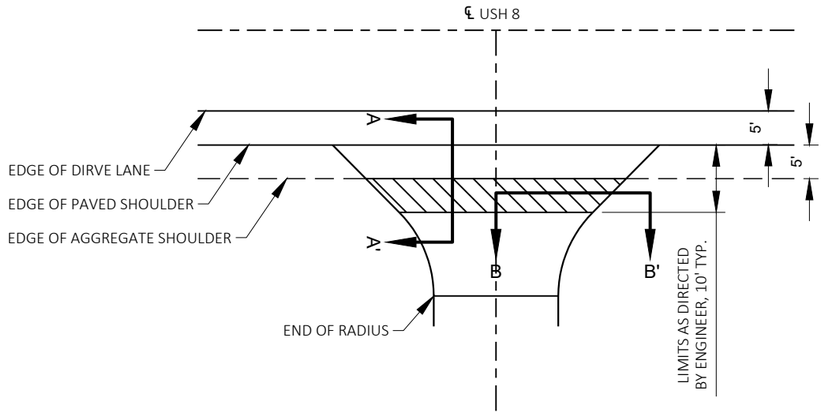
TYPICAL PAVED DRIVEWAY HALF SECTION (B-B')

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN: CONSTRUCTION DETAILS	SHEET 16	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_CD.DWG
LAYOUT NAME - CD-1

PLOT DATE : 10/8/2019 4:45 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 42

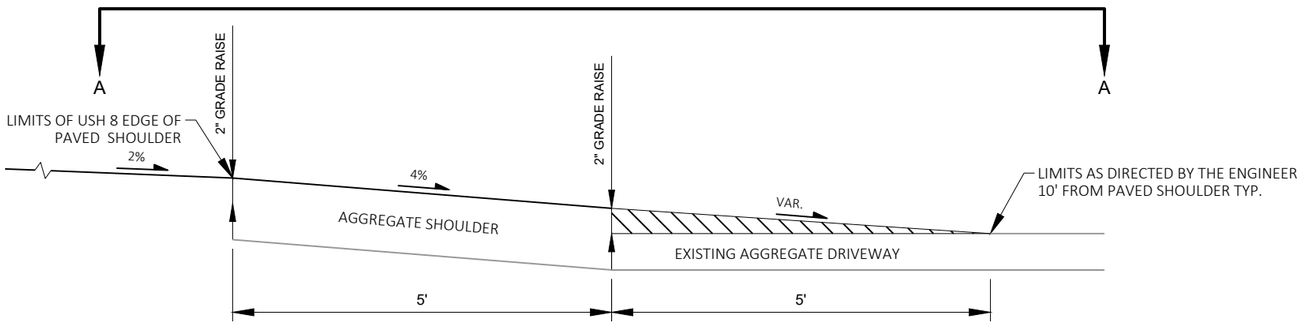


RESTORE AGGREGATE DRIVEWAY DETAIL

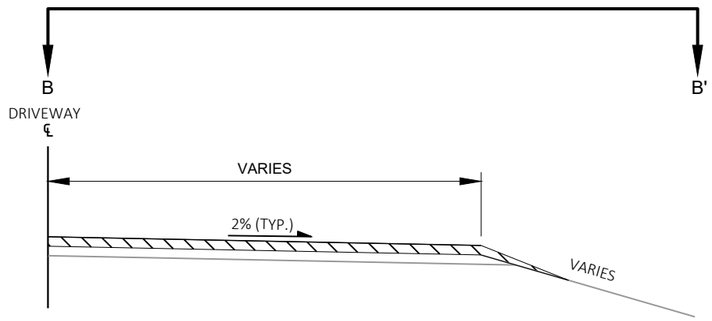
743+13 LT	795+30 LT	895+30 LT	918+90 RT	992+20 LT	1080+66 RT	1136+60 LT
743+26 RT	821+70 RT	899+80 LT	919+75 LT	992+20 RT	1083+14 LT	1150+75 RT
746+35 LT	828+45 LT	901+65 LT	924+85 RT	1018+00 RT	1085+62 LT	1150+85 LT
755+94 RT	872+67 LT	902+06 LT	926+60 LT	1022+00 LT	1089+76 RT	1198+88 RT
756+63 LT	874+00 RT	904+80 RT	942+80 LT	1032+50 RT	1102+96 LT	1201+80 LT
772+54 RT	878+70 LT	905+90 RT	957+20 LT	1051+42 LT	1105+90 RT	1206+35 RT
775+40 LT	885+00 LT	906+75 RT	957+40 RT	1065+10 LT	1108+90 RT	1208+90 LT
782+26 RT	888+32 LT	917+25 RT	972+35 LT	1065+85 RT	1118+20 LT	1240+55 LT
792+00 LT	890+50 RT	917+70 RT	988+00 LT	1080+50 LT	1130+95 RT	1242+05 LT

LEGEND

= EACH ENTRANCE SHALL RECEIVE ADEQUATE BASE AGGREGATE DENSE 3/4-INCH AFTER MAINLINE PAVING TO BRING ENTRANCE UP TO SHOULDER PAVEMENT GRADE. MATCH EXISTING DRIVEWAY WIDTH AND RADII.

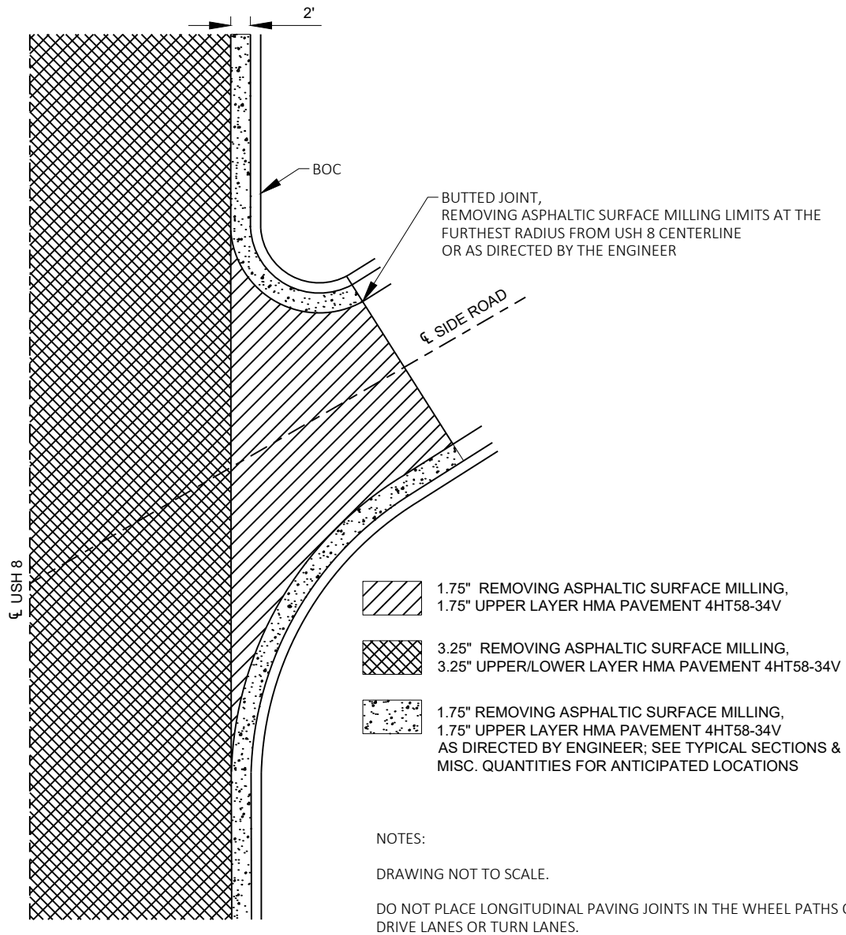


DETAIL OF RURAL AGGREGATE DRIVEWAY (A-A')



TYPICAL AGGREGATE DRIVEWAY HALF SECTION (B-B')

2



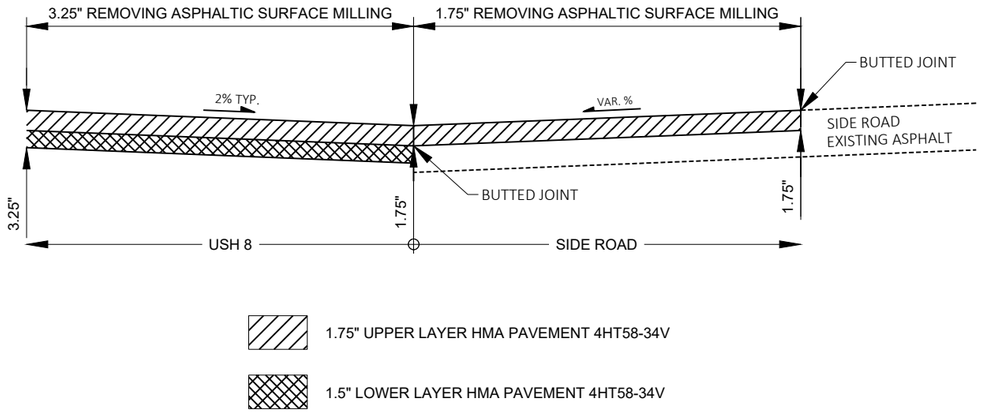
NOTES:

DRAWING NOT TO SCALE.

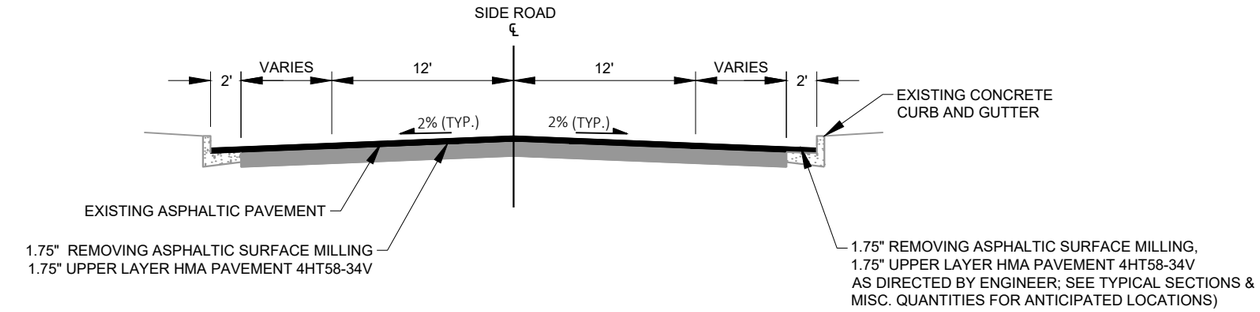
DO NOT PLACE LONGITUDINAL PAVING JOINTS IN THE WHEEL PATHS OF DRIVE LANES OR TURN LANES.

DETAIL OF PAVED SIDE ROAD - URBAN (NO MAINLINE GRADE RAISE)

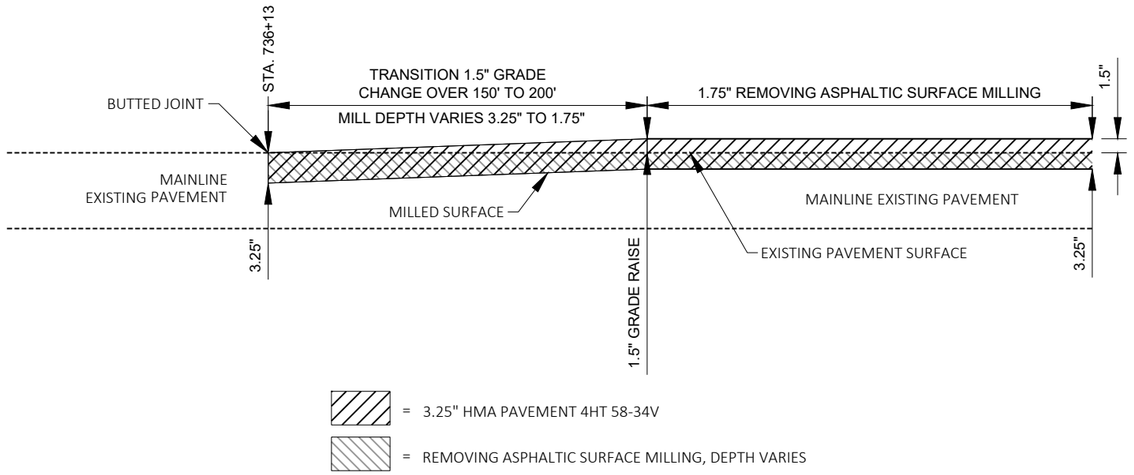
1217+39 LT (CTH T)
1217+33 RT (USH 63)
1230+76 RT (WESTERN BLVD)
1237+60 RT (PROSSER AVE)
1246+30 RT (PROSSER AVE)
1250+85 RT (PROSSER AVE)
1251+01 LT (FRONT AVE)



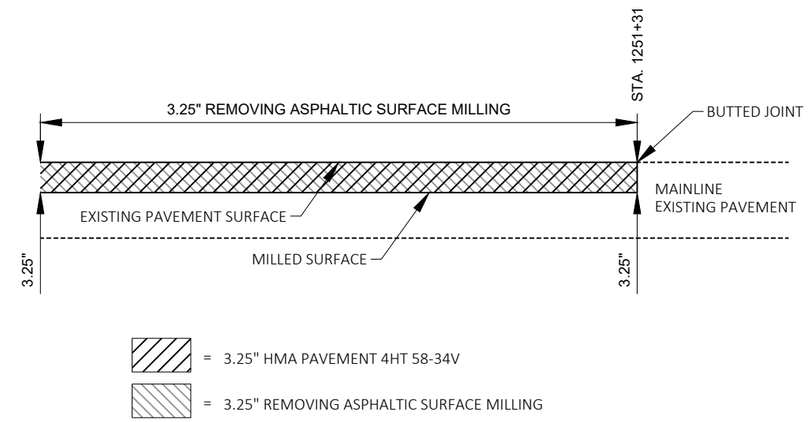
DETAIL OF PAVED SIDE ROAD BUTT JOINT - URBAN (NO GRADE RAISE)



TYPICAL PAVED SIDE ROAD SECTION - URBAN



DETAIL OF MAINLINE BUTT JOINT - BEGIN PROJECT (1.5" GRADE RAISE)



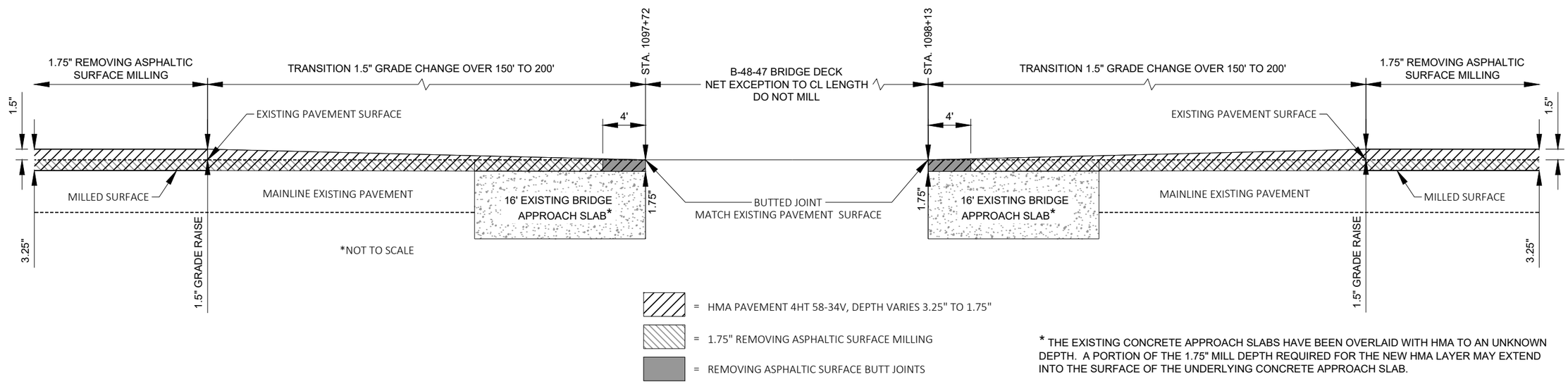
DETAIL OF MAINLINE BUTT JOINT - END PROJECT (NO GRADE RAISE)

NOTES:

NOT TO SCALE

EXACT DIMENSIONS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

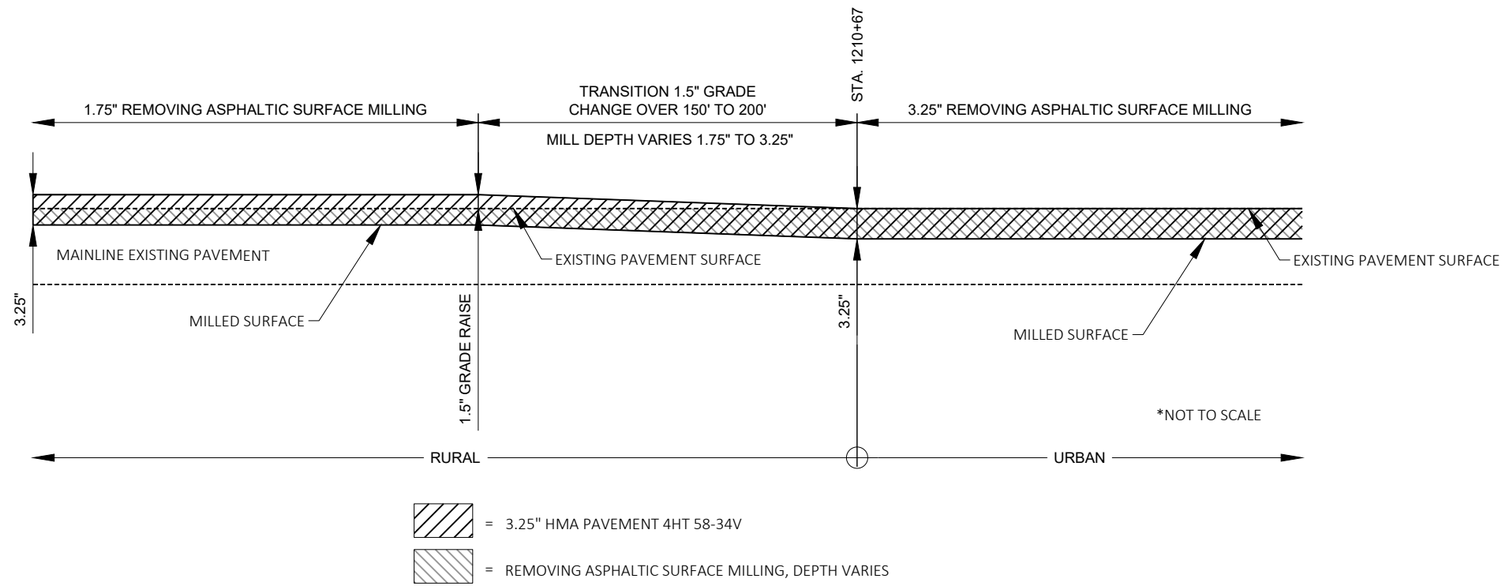
INSTALLATION AND REMOVAL OF TEMPORARY BUTT JOINT RAMP OR WEDGE IS INCIDENTAL TO THE WORK. ASPHALTIC SURFACE PATCHING REQUIRED FOR TEMPORARY BUTT JOINT RAMPS OR WEDGES WILL BE PAID FOR SEPARATELY.



* THE EXISTING CONCRETE APPROACH SLABS HAVE BEEN OVERLAID WITH HMA TO AN UNKNOWN DEPTH. A PORTION OF THE 1.75" MILL DEPTH REQUIRED FOR THE NEW HMA LAYER MAY EXTEND INTO THE SURFACE OF THE UNDERLYING CONCRETE APPROACH SLAB.

DETAIL OF MAINLINE BUTT JOINT AT BRIDGE

2

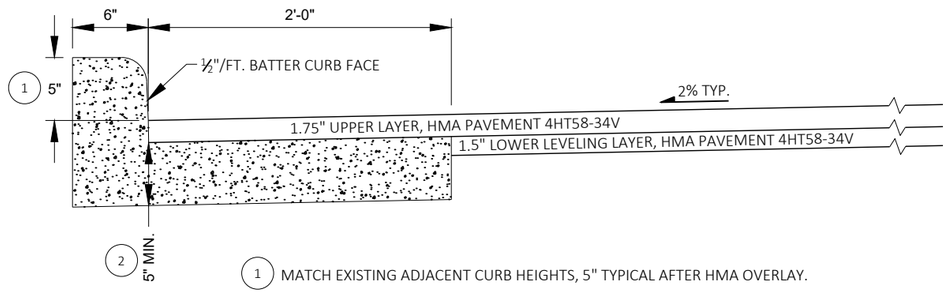


DETAIL OF PAVEMENT GRADE TRANSITION - RURAL TO URBAN

NOTES:

EXACT DIMENSIONS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

INSTALLATION AND REMOVAL OF TEMPORARY BUTT JOINT RAMP OR WEDGE IS INCIDENTAL TO THE WORK. ASPHALTIC SURFACE PATCHING REQUIRED FOR TEMPORARY BUTT JOINT RAMPS OR WEDGES WILL BE PAID FOR SEPARATELY.

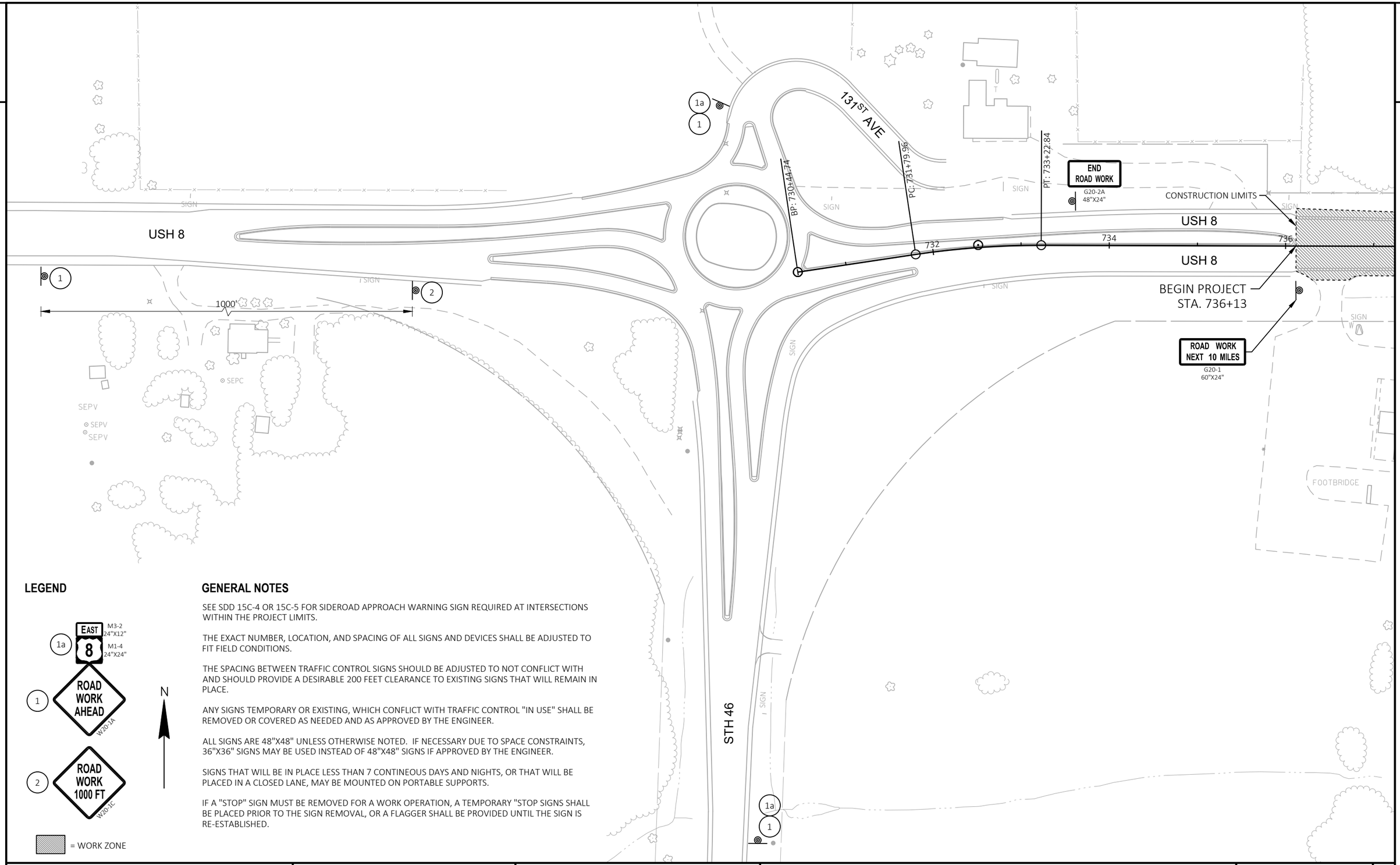


1. MATCH EXISTING ADJACENT CURB HEIGHTS, 5" TYPICAL AFTER HMA OVERLAY.
2. WHERE NEW CONCRETE CURB AND GUTTER IS BEING PLACED IN LOCATIONS WHERE ADJACENT GUTTER PAN IS BEING OVERLAYED, REDUCE TYPICAL THICKNESS OF TYPE D CONCRETE GUTTER PAN TO ALLOW PLACEMENT OF A 1.75" UPPER LAYER HMA PAVEMENT.

   THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE OVERLYING HMA PAVEMENT; MINIMUM CONCRETE THICKNESS = 5".

DETAIL OF NEW CONCRETE CURB & GUTTER WITH HMA OVERLAY (MODFIED 30" TYPE D SHOWN)

2



LEGEND

1a — EAST 8 — M3-2 24"X12" / M1-4 24"X24"

1 — ROAD WORK AHEAD — W20-1A

2 — ROAD WORK 1000 FT — W20-1C

= WORK ZONE

N

GENERAL NOTES

SEE SDD 15C-4 OR 15C-5 FOR SIDEROAD APPROACH WARNING SIGN REQUIRED AT INTERSECTIONS WITHIN THE PROJECT LIMITS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

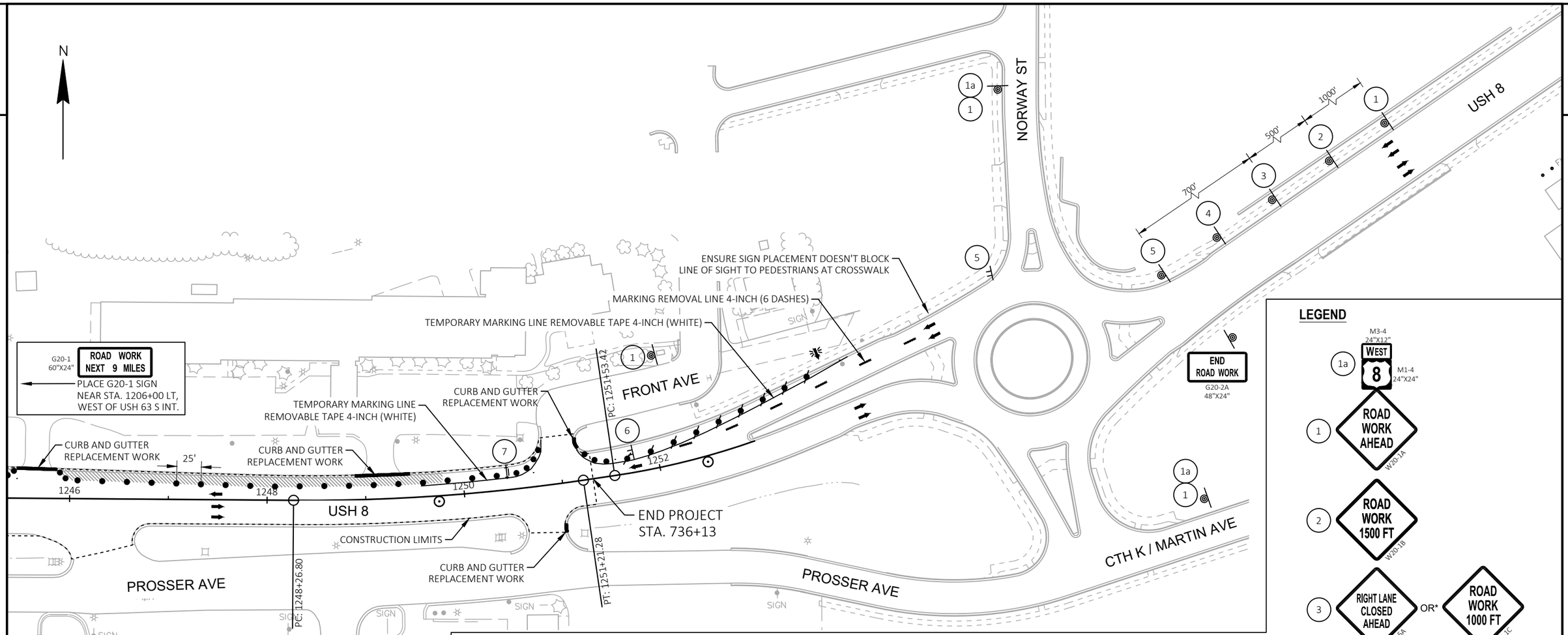
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48"X48" SIGNS IF APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINEOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP SIGNS SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	TRAFFIC CONTROL - BEGIN PROJECT ADVANCED WARNING	SHEET 21	E



GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

WHEN THE LANE CLOSURE IS NOT IN PLACE, REPLACE THE W20-5A AND W4-2R SIGNS WITH W20-1C AND W20-1(MOD) SIGNS, RESPECTIVELY.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48"X48" SIGNS IF APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINEOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP SIGNS SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

SEE SDD 'TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY' FOR ADDITIONAL LANE CLOSURE TRAFFIC CONTROL REQUIRED WEST OF THIS DETAIL SHEET.

SEE SDD 15C-4 OR 15C-5 FOR SIDEROAD APPROACH WARNING SIGN REQUIRED AT INTERSECTIONS WITHIN THE PROJECT LIMITS.

- WORK ZONE FOR SPOT CURB AND GUTTER REPLACEMENT
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- FLASHING ARROW BOARD

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	TRAFFIC CONTROL - END PROJECT ADVANCED WARNING AND SINGLE LANE CLOSURE	SHEET 22	E

LEGEND:

(1) = PLACE 'BUMP' SIGN AT PROFILE CHANGE EXCEEDING ONE INCH.

= UPPER LAYER HMA

= LOWER LAYER HMA

= MILLED SURFACE

= SHOULDER WIDENING

= DIRECTION OF TRAFFIC

= SIGN ON TEMPORARY SUPPORT

NOTES:

DRAWING NOT TO SCALE.

PLACE 'UNEVEN LANES' AND 'DO NOT PASS' SIGNS WHEN LONGITUDINAL ELEVATION CHANGES OF 1/2-INCH OR GREATER ARE LEFT OVERNIGHT. PLACE ADDITIONAL SIGNS AT 1-MILE INTERVALS OR AD DIRECTED BY THE ENGINEER.

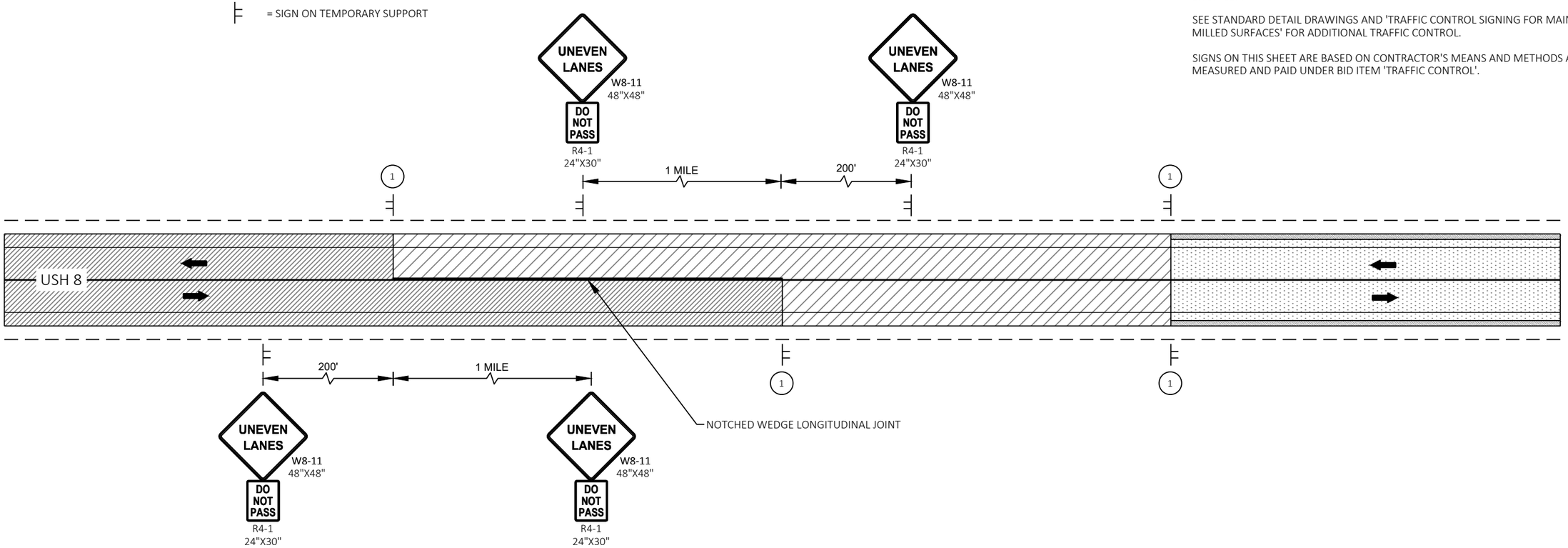
THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS SPECIFIED IN THE PLANS, THE SPECIAL PROVISIONS, AND/OR AS DIRECTED BY THE ENGINEER.

SEE STANDARD DETAIL DRAWINGS AND 'TRAFFIC CONTROL SIGNING FOR MAINLINE MILLED SURFACES' FOR ADDITIONAL TRAFFIC CONTROL.

SIGNS ON THIS SHEET ARE BASED ON CONTRACTOR'S MEANS AND METHODS AND ARE MEASURED AND PAID UNDER BID ITEM 'TRAFFIC CONTROL'.



TRAFFIC CONTROL SIGNING FOR UNEVEN SURFACES

NOTES:

DRAWING NOT TO SCALE.

PLACE 'GROOVED PAVEMENT' AND '45 MPH' SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1-MILE INTERVALS OR AD DIRECTED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

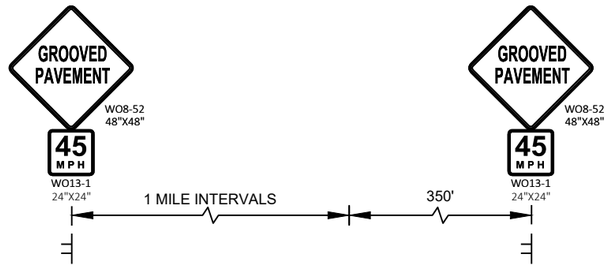
"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

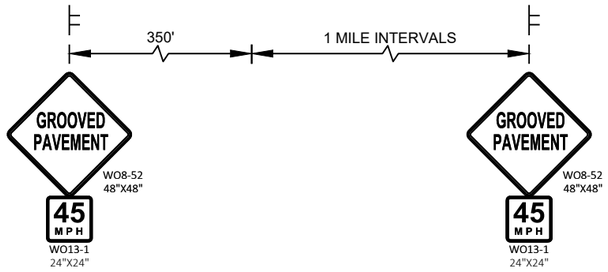
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS SPECIFIED IN THE PLANS, THE SPECIAL PROVISIONS, AND/OR AS DIRECTED BY THE ENGINEER.

'45 MPH' (WO13-1) SIGNS ONLY REQUIRED WHERE THE POSTED SPEED IS GREATER THAN 45 MPH.

SEE STANDARD DETAIL DRAWINGS AND 'TRAFFIC CONTROL SIGNING FOR UNEVEN SURFACES' FOR ADDITIONAL TRAFFIC CONTROL.

SIGNS ON THIS SHEET SHALL BE MEASURED AND PAID FOR WITH THE 'TRAFFIC CONTROL SIGNS' BID ITEM.





TRAFFIC CONTROL SIGNING FOR MAINLINE MILLED SURFACES

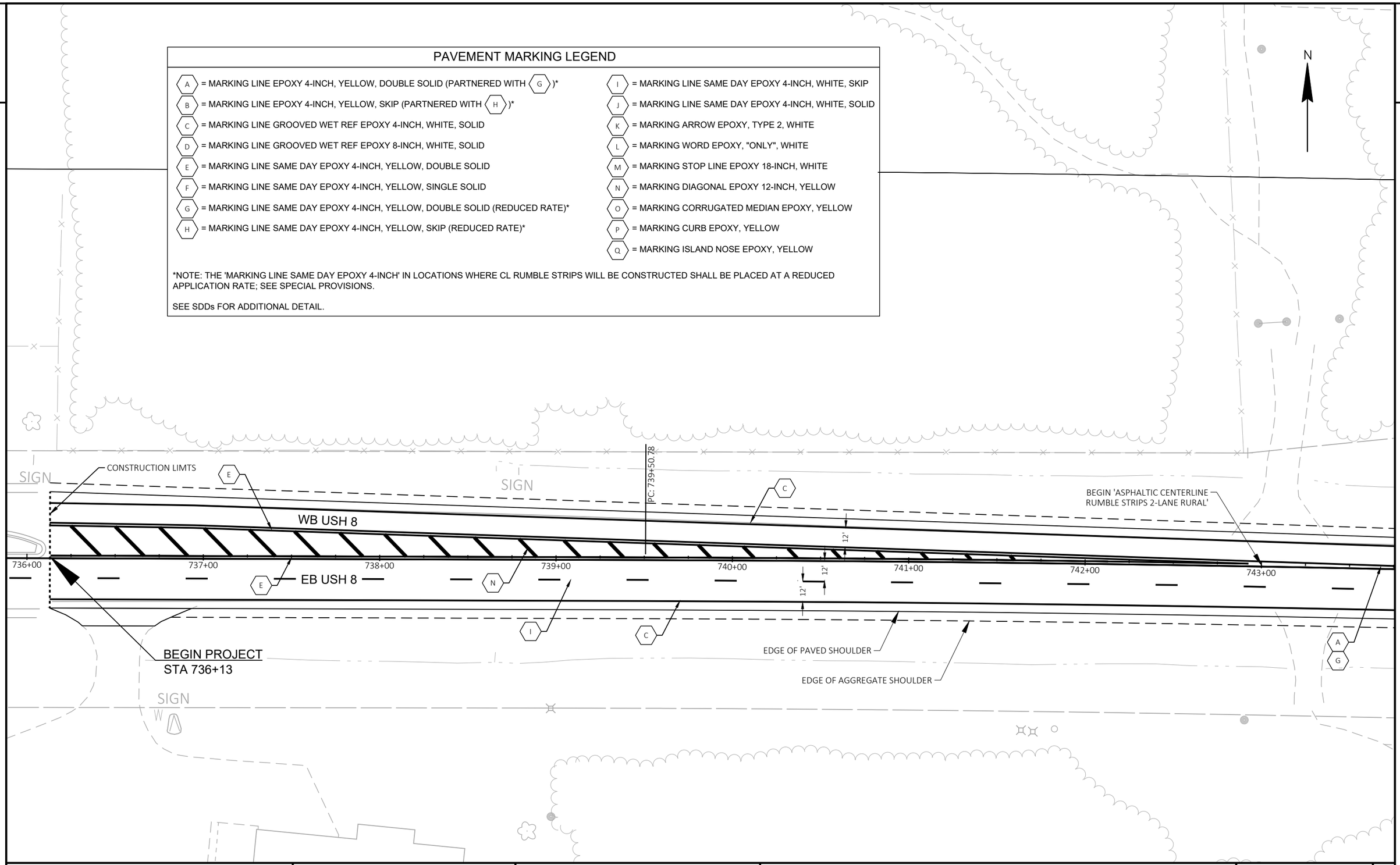
PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	TRAFFIC CONTROL	SHEET 24	E

PAVEMENT MARKING LEGEND

- (A) = MARKING LINE EPOXY 4-INCH, YELLOW, DOUBLE SOLID (PARTNERED WITH (G))*
- (B) = MARKING LINE EPOXY 4-INCH, YELLOW, SKIP (PARTNERED WITH (H))*
- (C) = MARKING LINE GROOVED WET REF EPOXY 4-INCH, WHITE, SOLID
- (D) = MARKING LINE GROOVED WET REF EPOXY 8-INCH, WHITE, SOLID
- (E) = MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW, DOUBLE SOLID
- (F) = MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW, SINGLE SOLID
- (G) = MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW, DOUBLE SOLID (REDUCED RATE)*
- (H) = MARKING LINE SAME DAY EPOXY 4-INCH, YELLOW, SKIP (REDUCED RATE)*
- (I) = MARKING LINE SAME DAY EPOXY 4-INCH, WHITE, SKIP
- (J) = MARKING LINE SAME DAY EPOXY 4-INCH, WHITE, SOLID
- (K) = MARKING ARROW EPOXY, TYPE 2, WHITE
- (L) = MARKING WORD EPOXY, "ONLY", WHITE
- (M) = MARKING STOP LINE EPOXY 18-INCH, WHITE
- (N) = MARKING DIAGONAL EPOXY 12-INCH, YELLOW
- (O) = MARKING CORRUGATED MEDIAN EPOXY, YELLOW
- (P) = MARKING CURB EPOXY, YELLOW
- (Q) = MARKING ISLAND NOSE EPOXY, YELLOW

*NOTE: THE 'MARKING LINE SAME DAY EPOXY 4-INCH' IN LOCATIONS WHERE CL RUMBLE STRIPS WILL BE CONSTRUCTED SHALL BE PLACED AT A REDUCED APPLICATION RATE; SEE SPECIAL PROVISIONS.

SEE SDDs FOR ADDITIONAL DETAIL.



PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PAVEMENT MARKING	SHEET 25	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PM.DWG
LAYOUT NAME - PM-1
PLOT DATE : 10/8/2019 4:47 PM
PLOT BY : ERICKSON, ZACHARY A
PLOT NAME :
PLOT SCALE : 1 IN:50 FT
WISDOT/CADDS SHEET 42

2

N

PI: 1204+98.00

END 'ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL'

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

WB USH 8

EB USH 8

12'

+00 1206+00 1207+00 1208+00 1209+00 1210+00 1211+00 1212+00

A G C E N Q O C F F

SIGN SIGN

SEE LEGEND ON FIRST PAVEMENT MARKING PLAN

N

CTH T

CONSTRUCTION LIMTS

E M

SIGN SIGN

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

C I C

F K D L K P F K L F WB USH 8 D

EXISTING RAISED MEDIAN

12' 12' 14' 10' 2' 22' 34'

1213+00 1214+00 1215+00 1216+00 1217+00 1218+00 1219+00 EB USH 8 1220+00

K F D C L K F

SIGN

M E USH 63 CONSTRUCTION LIMTS

C CONSTRUCTION LIMTS NB USH 63 J C

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PAVEMENT MARKING	SHEET 26	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PM.DWG
LAYOUT NAME - PM-2

PLOT DATE : 10/8/2019 4:47 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 44

2

2

N

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

SIGN

SIGN

C

I

D

K

F

WB USH 8

12'

14'

10'

12'

P

34'

EXISTING RAISED MEDIAN

EXISTING RAISED MEDIAN

1221+00

1222+00

1223+00

1224+00

1225+00

1226+00

1227+00

1228+00

EB USH 8

2'

12'

J

16'

C

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

SIGN

SIGN

SIGN

CONSTRUCTION LIMTS

I

12'

K

F

D

SIGN

MH

SEE LEGEND ON FIRST PAVEMENT MARKING PLAN

PAVEMENT MARKING PLAN

N

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

C

I

OFFSET TO WB
INSIDE LANE LINE
1236+00.00, 22.00' LT

F

12'

12'

WB USH 8

EXISTING RAISED MEDIAN

P

34'

D

K

K

14'

10'

EXISTING RAISED MEDIAN

1229+00

1230+00

1231+00

1232+00

1233+00

1234+00

1235+00

1236+00

2'

F

12'

12'

EB USH 8

F

D

12'

M

L

K

C

C

I

K

SLAB

WESTERN BLVD

CONSTRUCTION LIMTS

SIGN

SIGN

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PAVEMENT MARKING	SHEET 27	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PM.DWG
LAYOUT NAME - PM-3

PLOT DATE : 10/8/2019 4:48 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 44

2

2

N

OFFSET TO WB
INSIDE LANE LINE
1237+00.00, 21.93' LT

OFFSET TO WB
INSIDE LANE LINE
1238+00.00, 21.49' LT

OFFSET TO WB
INSIDE LANE LINE
1239+00.00, 20.56' LT

OFFSET TO WB
INSIDE LANE LINE
1240+00.00, 19.22' LT

OFFSET TO WB
INSIDE LANE LINE
1241+00.00, 17.45' LT

OFFSET TO WB
INSIDE LANE LINE
1242+00.00, 15.24' LT

OFFSET TO WB
INSIDE LANE LINE
1243+00.00, 12.60' LT

1244+00.

WB USH 8

EB USH 8

EXISTING RAISED MEDIAN

1237+00 1238+00 1239+00 1240+00 1241+00 1242+00 1243+00 1244+00

HEIGHT MOD

CONSTRUCTION LIMTS

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

PROSSER AVE

SEE LEGEND ON FIRST
PAVEMENT MARKING PLAN

FRONT AVE

OFFSET TO WB
INSIDE LANE LINE
1244+00.00, 9.86' LT

OFFSET TO WB
INSIDE LANE LINE
1245+00.00, 7.07' LT

OFFSET TO WB
INSIDE LANE LINE
1246+00.00, 4.34' LT

OFFSET TO WB
INSIDE LANE LINE
1247+00.00, 1.75' LT

OFFSET TO WB
INSIDE LANE LINE
1247+57.72, 0.00'

PC: 1248+26.80

PT: 1251+21.28

PC: 1251+53.42

1244+37.50

1245+00 1246+00 1247+00 1248+00 1249+00 1250+00 1251+00

WB USH 8

EB USH 8

CONSTRUCTION LIMTS

END PROJECT
STA. 1251+31

PROSSER AVE

N

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PAVEMENT MARKING	SHEET 28	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PM.DWG
LAYOUT NAME - PM-4

PLOT DATE : 10/8/2019 4:48 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 44

					1570-00-74	1570-00-77
Line	**Item**	**Item Description**	**Unit**	**Total**	**Qty**	**Qty**
0002	203.0100	Removing Small Pipe Culverts	EACH	1.000	1.000	
0004	204.0110	Removing Asphaltic Surface	SY	105.000	105.000	
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	392.000	392.000	
0008	204.0120	Removing Asphaltic Surface Milling	SY	236,396.000	236,396.000	
0010	204.0150	Removing Curb & Gutter	LF	192.000	192.000	
0012	204.0165	Removing Guardrail	LF	380.000	380.000	
0014	211.0100	Prepare Foundation for Asphaltic Paving (project) 01. 1570-00-74	LS	1.000	1.000	
0016	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	926.000	60.000	866.000
0018	213.0100	Finishing Roadway (project) 01. 1570-00-74	EACH	1.000	1.000	
0020	305.0110	Base Aggregate Dense 3/4-Inch	TON	11,502.000	11,502.000	
0022	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	19.000	19.000	
0024	455.0605	Tack Coat	GAL	29,618.000	28,668.000	950.000
0026	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	1.000	1.000	
0028	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	1.000	1.000	
0030	460.2005	Incentive Density PWL HMA Pavement	DOL	12,630.000	12,630.000	
0032	460.2010	Incentive Air Voids HMA Pavement	DOL	46,210.000	46,210.000	
0034	460.4110.S	Reheating HMA Pavement Longitudinal Joints	LF	84,456.000	84,456.000	
0036	460.7644	HMA Pavement 4 HT 58-34 V	TON	46,203.000	42,747.000	3,456.000
0038	465.0105	Asphaltic Surface	TON	400.000	400.000	
0040	465.0110	Asphaltic Surface Patching	TON	27.000	27.000	
0042	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	76.000	76.000	
0044	465.0425	Asphaltic Shoulder Rumble Strips 2-Lane Rural	LF	79,452.000		79,452.000
0046	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	41,630.000	41,630.000	
0048	520.8000	Concrete Collars for Pipe	EACH	1.000	1.000	
0050	522.0124	Culvert Pipe Reinforced Concrete Class III 24-Inch	LF	16.000	16.000	
0052	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	135.000	135.000	
0054	601.0557	Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type D	LF	57.000	57.000	
0056	614.2300	MGS Guardrail 3	LF	50.000	50.000	
0058	614.2500	MGS Thrie Beam Transition	LF	157.600	157.600	
0060	614.2610	MGS Guardrail Terminal EAT	EACH	4.000	4.000	
0062	618.0100	Maintenance And Repair of Haul Roads (project) 01. 1570-00-74	EACH	1.000	1.000	
0064	619.1000	Mobilization	EACH	1.000	0.900	0.100
0066	624.0100	Water	MGAL	173.000	173.000	
0068	633.5200	Markers Culvert End	EACH	10.000	10.000	
0070	642.5001	Field Office Type B	EACH	1.000	1.000	
0072	643.0300	Traffic Control Drums	DAY	3,584.000	3,584.000	
0074	643.0420	Traffic Control Barricades Type III	DAY	14.000	14.000	
0076	643.0715	Traffic Control Warning Lights Type C	DAY	70.000	70.000	

					1570-00-74	1570-00-77
Line	**Item**	**Item Description**	**Unit**	**Total**	**Qty**	**Qty**
0078	643.0800	Traffic Control Arrow Boards	DAY	7.000	7.000	
0080	643.0900	Traffic Control Signs	DAY	9,395.000	9,395.000	
0082	643.5000	Traffic Control	EACH	1.000	1.000	
0084	646.1020	Marking Line Epoxy 4-Inch	LF	57,751.000	57,751.000	
0086	646.1040	Marking Line Grooved Wet Ref Epoxy 4-Inch	LF	101,471.000	101,471.000	
0088	646.3040	Marking Line Grooved Wet Ref Epoxy 8-Inch	LF	5,065.000	5,065.000	
0090	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	78,134.000	78,134.000	
0092	646.5020	Marking Arrow Epoxy	EACH	44.000	44.000	
0094	646.5120	Marking Word Epoxy	EACH	17.000	17.000	
0096	646.6120	Marking Stop Line Epoxy 18-Inch	LF	179.000	179.000	
0098	646.7120	Marking Diagonal Epoxy 12-Inch	LF	451.000	451.000	
0100	646.8020	Marking Corrugated Median Epoxy	SF	836.000	836.000	
0102	646.8120	Marking Curb Epoxy	LF	300.000	300.000	
0104	646.8220	Marking Island Nose Epoxy	EACH	2.000	2.000	
0106	646.9000	Marking Removal Line 4-Inch	LF	75.000	75.000	
0108	649.0105	Temporary Marking Line Paint 4-Inch	LF	130,571.000	130,571.000	
0110	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	450.000	450.000	
0112	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	192.000	192.000	
0114	650.8000	Construction Staking Resurfacing Reference	LF	51,477.000	51,477.000	
0116	650.9910	Construction Staking Supplemental Control (project) 01. 1570-00-74	LS	1.000	1.000	
0118	690.0150	Sawing Asphalt	LF	325.000	325.000	
0120	690.0250	Sawing Concrete	LF	30.000	30.000	
0122	740.0440	Incentive IRI Ride	DOL	50,759.000	50,759.000	
0124	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	2,000.000	2,000.000	
0126	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	630.000	630.000	
0128	SPV.0090	Special 01. Concrete Curb and Gutter Cure and Seal Treatment	LF	192.000	192.000	
0130	SPV.0105	Special 01. Material Transfer Vehicle (Project 1570-00-74)	LS	1.000	1.000	
0132	SPV.0105	Special 02. Milling and Removing Temporary Joint (Project 1570-00-74)	LS	1.000	1.000	

3

REMOVING SMALL PIPE CULVERTS

CATEGORY	STATION	LOCATION	203.0100 EACH	REMARKS
PROJECT 1570-00-74				
0010	778+03	18'-34' RT	1	PARTIAL REMOVAL, SAWCUTS REQUIRED
PROJECT 1570-00-74 CATEGORY 0010 TOTAL			1	
CONTRACT CATEGORY 0010 TOTAL			1	

REMOVING ASPHALTIC SURFACE

CATEGORY	STATION	TO	STATION	LOCATION	204.0110 SY	REMARKS
PROJECT 1570-00-74						
0010	777+93	-	778+13	RT	30	FOR CULVERT REPAIR
0010	1216+75	-	1217+75	LT	15	FOR CURB AND GUTTER REPLACEMENT - CTH T SIDEROAD
0010	1241+94	-	1242+24	LT	15	FOR DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1245+46		1245+86	LT	18	FOR DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1248+90	-	1249+45	LT	24	FOR DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1251+15	-	1251+20	LT	3	FOR CURB & GUTTER REPLACEMENT - FRONT AVE, EAST RADIUS
PROJECT 1570-00-74 CATEGORY 0010 TOTAL					105	
CONTRACT CATEGORY 0010 TOTAL					105	

MILLING REMOVAL SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	LENGTH (FT)	WIDTH (FT)	SF	SY	MILLING DEPTH (IN)	REMOVING ASPHALTIC SURFACE BUTT JOINTS 204.0115 SY	REMOVING ASPHALTIC SURFACE MILLING 204.0120 SY	REMARKS
PROJECT 1570-00-74												
RURAL SEGMENT												
0010	736+13	-	744+00	LT & RT	787	64-42	39870	4430	1.75	-	4430	DRIVE LANES, PAVED MEDIAN, EXISTING SHOULDER WIDTH
0010	744+00	-	794+00	LT & RT	5000	42	210000	23333	1.75	-	23333	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	794+00	-	801+00	LT & RT	700	42-30	25200	2800	1.75	-	2800	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	801+00	-	815+50	LT & RT	1450	30	43500	4833	1.75	-	4833	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	815+50	-	822+50	LT & RT	700	30-42	25200	2800	1.75	-	2800	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	822+50	-	884+00	LT & RT	6150	42	258300	28700	1.75	-	28700	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	884+00	-	891+00	LT & RT	700	42-30	25200	2800	1.75	-	2800	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	891+00	-	907+80	LT & RT	1680	30	50400	5600	1.75	-	5600	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	907+80	-	915+50	LT & RT	770	24	18480	2053	1.75	-	2053	RANGE - DIRVE LANES (SEE BELOW FOR SHOULDER/TURN LANE QTY.)
0010	915+50	-	993+00	LT & RT	7750	30	232500	25833	1.75	-	25833	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	993+00	-	1000+00	LT & RT	700	30-42	25200	2800	1.75	-	2800	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1000+00	-	1056+00	LT & RT	5600	42	235200	26133	1.75	-	26133	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	1056+00	-	1063+00	LT & RT	700	42-30	25200	2800	1.75	-	2800	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1063+00	-	1097+68	LT & RT	3468	30	104040	11560	1.75	-	11560	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	1097+68	-	1097+72	LT & RT	4	46	184	20	1.75	20	-	MAINLINE BUTT JOINT - BRIDGE
0010	1098+13	-	1098+17	LT & RT	4	46	184	20	1.75	20	-	MAINLINE BUTT JOINT - BRIDGE
0010	1098+17	-	1112+00	LT & RT	1383	30	41490	4610	1.75	-	4610	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	1112+00	-	1119+50	LT & RT	750	30-42	27000	3000	1.75	-	3000	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1119+50	-	1185+00	LT & RT	6550	42	275100	30567	1.75	-	30567	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	1185+00	-	1192+00	LT & RT	700	42-30	25200	2800	1.75	-	2800	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1192+00	-	1206+28	LT & RT	1428	30	42840	4760	1.75	-	4760	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	1206+28	-	1210+67	LT & RT	439	30-47	16311	1812	1.75	-	1812	EXISTING PAVEMENT LIMITS
RURAL RT. TURN LANES & TAPERS BEYOND TYPICAL SHOULDERS												
0010	804+74	-	807+79	RT	-	-	2907	255	1.75	-	255	80TH ST.
0010	807+79	-	814+12	RT	-	-	5822	647	1.75	-	647	80TH ST.
0010	807+18	-	812+35	LT	-	-	4653	446	1.75	-	446	CTH E
0010	821+42	-	828+12	RT	-	-	6298	600	1.75	-	600	CE
0010	855+67	-	862+38	RT	-	-	5743	638	1.75	-	638	70TH ST.
0010	859+61	-	864+91	LT	-	-	4804	458	1.75	-	458	70TH ST.
0010	906+62	-	915+50	RT	-	-	9721	1053	1.75	-	1053	RANGE SHOULDER/RT. TURN LANE
0010	907+28	-	916+61	LT	-	-	9491	1033	1.75	-	1033	RANGE SHOULDER/RT. TURN LANE
0010	936+04	-	938+87	RT	-	-	1769	181	1.75	-	181	56TH ST.
0010	973+52	-	978+73	RT	-	-	4629	450	1.75	-	450	50TH ST.
0010	975+91	-	981+25	LT	-	-	4698	454	1.75	-	454	50TH ST.
0010	1068+02	-	1073+96	RT	-	-	4908	490	1.75	-	490	33RD ST.
0010	1069+65	-	1074+88	LT	-	-	4538	436	1.75	-	436	CTH V / 33RD ST.
0010	1092+97	-	1097+68	LT & RT	-	-	4496	500	1.75	-	500	WIDENED SHOULDERS NEAR BRIDGE APPROACH
0010	1098+17	-	1099+80	LT & RT	-	-	1935	215	1.75	-	215	WIDENED SHOULDERS NEAR BRIDGE APPROACH
0010	1000+30	-	1104+50	RT	-	-	3588	351	1.75	-	351	125TH AVE.
0010	1160+62	-	1166+02	RT	-	-	5120	497	1.75	-	497	15TH ST.
0010	1163+16	-	1168+25	LT	-	-	4392	420	1.75	-	420	15TH ST.
0010	-		-	LT	-	-	-	-	VAR.	352	-	BUTT JOINTS FOR RESTORING 14 RURAL PAVED DRIVEWAYS
URBAN SEGMENT												
0010	1210+67	-	1251+31	LT & RT	4064	VAR.	276603	30627	3.25	-	30627	EXIST. PAVEMENT LIMITS (EXCLUDES SIDE ROAD AREA)
0010	1210+67	-	1216+61	EB MEDIAN	594	1.67	992	110	1.75	-	110	MEDIAN GUTTER PAN
0010	1210+67	-	1216+61	WB MEDIAN	594	1.67	992	110	1.75	-	110	MEDIAN GUTTER PAN
0010	1217+80	-	1225+12	WB MEDIAN	732	1.67	1222	136	1.75	-	136	MEDIAN GUTTER PAN
0010	1223+62	-	1229+72	EB RT	610	2.00	1220	136	1.75	-	136	GUTTER PAN
0010	1231+12	-	1237+26	WB MEDIAN	614	1.67	1025	114	1.75	-	114	MEDIAN GUTTER PAN
0010	1238+10	-	1241+70	WB MEDIAN	360	1.67	601	67	1.75	-	67	MEDIAN GUTTER PAN
0010	1240+70	-	1250+46	WB RT	976	2.00	1952	217	1.75	-	217	GUTTER PAN
0010	1216+62	-	1218+01	SIDE RD, LT	139	VAR.	3154	350	1.75	-	350	CTH T
0010	1216+69	-	1217+76	SIDE RD, RT	107	VAR.	2056	228	1.75	-	228	USH 63
0010	1229+71	-	1231+36	SIDE RD, RT	165	VAR.	5099	567	1.75	-	567	WESTERN BLVD.
0010	1237+15	-	1238+02	SIDE RD, RT	87	VAR.	1315	146	1.75	-	146	PROSSER AVE.
0010	1245+64	-	1246+79	SIDE RD, RT	115	VAR.	1930	214	1.75	-	214	PROSSER AVE.
0010	1250+44	-	1251+17	SIDE RD, RT	73	VAR.	1003	111	1.75	-	111	PROSSER AVE.
0010	1250+49	-	1251+31	SIDE RD, LT	82	VAR.	1034	115	1.75	-	115	FRONT ST.
									PROJECT 1570-00-74 CATEGORY 0010 TOTAL	392	236396	
									CONTRACT CATEGORY 0010 TOTAL	392	236396	

REMOVING CURB & GUTTER

CATEGORY	STATION	TO	STATION	LOCATION	204.0150 LF	REMARKS
PROEJCT 1570-00-74						
0010	1216+75	-	1216+95	LT	25	CTH T - NW RADIUS
0010	1217+47	-	1217+76	LT	32	CTH T - NE RADIUS
0010	1241+94	-	1242+24	LT	30	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1245+46	-	1245+86	LT	40	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1248+90	-	1249+45	LT	55	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1251+15	-	1251+20	LT	10	FRONT AVE - EAST RADIUS
PROJECT 1570-00-74 CATEGORY 0010 TOTAL					192	
CONTRACT CATEGORY 0010 TOTAL					192	

REMOVING GUARDRAIL

CATEGORY	STATION	TO	STATION	LOCATION	204.0165 LF	REMARKS
PROJECT 1570-00-74						
0010	1096+77	-	1097+72	LT	95	B-48-47, NW QUADRANT
0010	1098+13	-	1099+08	LT	95	B-48-47, NE QUADRANT
0010	1096+77	-	1097+72	RT	95	B-48-47, SW QUADRANT
0010	1098+13	-	1099+08	RT	95	B-48-47, SE QUADRANT
PROJECT 1570-00-74 CATEGORY 0010 TOTAL					380	
CONTRACT CATEGORY 0010 TOTAL					380	

PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT 1570-00-74)

CATEGORY	LOCATION	211.0100 LS
PROJECT 1570-00-74		
0010	PROJECT 1570-00-74	1
PROJECT 1570-00-74 CATEGORY 0010 TOTAL		1
CONTRACT CATEGORY 0010 TOTAL		1

PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS

211.0400

CATEGORY	STATION	TO	STATION	LENGTH (FT)	LOCATION	STA	REMARKS
PROJECT 1570-00-74							FOR 2' SHOULDER WIDENING ADJACENT TO RIGHT TURN LANES
0010	809+29	-	812+35	306	LT	4	RT. TURN LANE FOR CTH E
0010	861+80	-	864+91	311	LT	4	RT. TURN LANE FOR 70TH ST.
0010	915+48	-	916+61	113	LT	2	RT. TURN LANE FOR CTH D / 60TH ST.
0010	978+09	-	981+25	316	LT	4	RT. TURN LANE FOR 50TH ST.
0010	1071+70	-	1074+88	318	LT	4	RT. TURN LANE FOR CTH V / 33RD ST.
0010	1165+21	-	1168+25	304	LT	4	RT. TURN LANE FOR 15TH ST.
0010	804+69	-	807+80	311	RT	4	RT. TURN LANE FOR 80TH ST.
0010	821+42	-	826+04	462	RT	5	RT. TURN LANE FOR CE
0010	855+67		856+81	114	RT	2	RT. TURN LANE FOR 70TH ST.
0010	906+62	-	907+68	106	RT	2	RT. TURN LANE FOR RANGE
0010	936+04	-	936+92	88	RT	1	RT. TURN LANE FOR 56TH ST.
0010	973+52	-	976+63	311	RT	4	RT. TURN LANE FOR 50TH ST.
0010	1068+02	-	1070+24	222	RT	3	RT. TURN LANE FOR 33RD ST.
0010	1092+97		1094+10	113	RT	2	RT. TAPER TO CURB & GUTTER
0010	1100+30	-	1102+34	204	RT	3	RT. TURN LANE FOR 125TH AVE.
0010	1160+62	-	1163+75	313	RT	4	RT. TURN LANE FOR 15TH ST.
0010	1211+95	-	1216+69	474	RT	5	RT. TURN LANE FOR USH 63
0010	1242+91	-	1245+70	279	RT	3	RT. TURN LANE FOR PROSSER AVE.
			PROJECT 1570-00-74 CATEGORY 0010 TOTAL			60	
PROJECT 1570-00-77							FOR WIDENING OF TYPICAL SHOULDERS FROM 3' TO 5' PAVED
0010	736+13	-	807+18	7105	LT	72	STH 46 TO CTH E
0010	812+35	-	859+61	4726	LT	48	CTH E TO 70TH ST.
0010	864+91	-	907+28	4237	LT	43	70TH ST. TO RANGE
0010	916+61	-	975+91	5930	LT	60	60TH/CTH D TO 50TH ST.
0010	981+25	-	1069+65	8840	LT	89	50TH TO 33RD ST./CTH V
0010	1074+88	-	1096+05	2117	LT	22	33RD ST./CTH V TO WEST OF BRIDGE APPROACH
0010	1099+80	-	1163+16	6336	LT	64	EAST OF BRIDGE APPROACH TO 15TH ST.
0010	1168+25	-	1215+95	4770	LT	48	15TH ST. TO CTH T
0010	736+13	-	804+69	6856	RT	69	STH 46 TO 80TH ST.
0010	814+32	-	821+42	710	RT	8	80TH ST. TO CE
0010	828+12	-	855+67	2755	RT	28	CE TO 70TH ST.
0010	862+38	-	906+62	4424	RT	45	70TH ST. TO RANGE
0010	915+07	-	936+04	2097	RT	21	60TH/CTH D TO 56TH ST.
0010	938+87	-	973+52	3465	RT	35	56TH ST. TO 50TH ST.
0010	978+73	-	1068+02	8929	RT	90	50TH ST. TO 33RD ST.
0010	1073+96	-	1092+97	1901	RT	20	33RD ST. TO WEST OF BRIDGE APPROACH
0010	1099+80	-	1100+30	50	RT	1	EAST OF BRIDGE APPROACH TO 125TH AVE.
0010	1104+50	-	1160+62	5612	RT	57	125TH AVE. TO 15TH ST.
0010	1166+02	-	1211+95	4593	RT	46	15TH ST. TO USH 63
			PROJECT 1570-00-77 CATEGORY 0010 TOTAL			866	
			CONTRACT CATEGORY 0010 TOTAL			926	

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FINISHING ROADWAY (PROJECT 1570-00-74)

CATEGORY	LOCATION	213.0100 EACH
PROJECT 1570-00-74		
0010	PROJECT 1570-00-74	1
	PROJECT 1570-00-74 CATEGORY 0010 TOTAL	1
	CONTRACT CATEGORY 0010 TOTAL	1

BASE AGGREGATE SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	BASE AGGREGATE DENSE 3/4-INCH 305.0110 TON	BASE AGGREGATE DENSE 1 1/4-INCH 305.0120 TON	WATER 624.0100 MGAL	REMARKS
PROJECT 1570-00-74								
0010	736+13	-	907+80	LT & RT	4195	-	62.9	SHOULDER AGGREGATE
0010	915+50	-	1097+72	LT & RT	4453	-	66.8	SHOULDER AGGREGATE
0010	1098+13	-	1206+28	LT & RT	2643	-	39.6	SHOULDER AGGREGATE
0010	1206+28	-	1210+67	LT & RT	81	-	1.2	SHOULDER AGGREGATE
0010	777+90	-	778+10	RT	-	19	0.3	CULVERT REPAIR AREA
0010	-		-	LT & RT	130	-	2.0	RESTORE PE, FE, CE; SEE CD FOR LOCATIONS
	PROJECT 1570-00-74 CATEGORY 0010 TOTAL				11502	19	173	
	CONTRACT CATEGORY 0010 TOTAL				11502	19	173	

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VOIDS & DENSITY TEST STRIP SUMMARY SUMMARY

CATEGORY	LOCATION	HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP VOLUMETRICS 460.0105.S EACH	HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP DENSITY 460.0110.S EACH	REMARKS
PROJECT 1570-00-74				
0010	LEVELING LAYER LAYER 4 HT 58-34V	1	-	SEE PWL MIXTURE USE TABLE
0010	UPPER LAYER 4 HT 58-34V	-	1	SEE PWL MIXTURE USE TABLE
	PROJECT 1570-00-74 CATEGORY 0010 TOTAL	1	1	
	CONTRACT CATEGORY 0010 TOTAL	1	1	

PWL MIXTURE USE TABLE

THE FOLLOWING ACCEPTANCE CRITERIA ARE APPLICABLE FOR THIS PROJECT:

LOCATION	STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	TYPICAL THICKNESS	QUALITY MANAGEMENT PROGRAM TO BE USED FOR: MIXTURE ACCEPTANCE	QUALITY MANAGEMENT PROGRAM TO BE USED FOR: DENSITY ACCEPTANCE
DRIVE LANES & PASSING LANES	736+13 TO 1251+31	UPPER LAYER	4 HT 58-34V	4 HT 58-34V	12627	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
DRIVE LANES & PASSING LANES	736+13 TO 1251+31	LEVELING LAYER	ASPHALTIC CONC. PAVEMENT E-10 OR RECYCLED ASPHALT SURFACE	4 HT 58-34V	10820	1.5"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE BY ORDINARY COMPACTION
MEDIANS, SHOULDERS, SIDEROADS & TURN LANES	736+13 TO 1251+31	UPPER LAYER	4 HT 58-34V	4 HT 58-34V	12253	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
MEDIANS, SHOULDERS & TURN LANES	736+13 TO 1251+31	LEVELING LAYER	ASPHALTIC CONC. PAVEMENT E-10, RECYCLED ASPHALT SURFACE, OR BASE AGGREGATE	4 HT 58-34V	10503	1.5"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE BY ORDINARY COMPACTION
HMA PAVING FOUNDATION	UNDISTRIBUTED	LEVELING AND WEDGING	VARIES	ASPHALTIC SURFACE	400	VARIES	QMP AS PER STANDARD SPEC. 465	ACCEPTANCE BY ORDINARY COMPACTION
PAVED DRIVEWAYS	VARIOUS	SINGLE LAYER	BASE AGGREGATE	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES	76	VARIES	QMP AS PER STANDARD SPEC. 465	ACCEPTANCE BY ORDINARY COMPACTION
MINOR REPAIRS	VARIOUS	ASPHALTIC PATCHING	BASE AGGREGATE	ASPHALTIC SURFACE PATCHING	27	VARIES	QMP AS PER STANDARD SPEC. 465	ACCEPTANCE BY ORDINARY COMPACTION

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REHEATING HMA PAVEMENT LONGITUDINAL JOINTS

CATEGORY	STATION	TO	STATION	LOCATION	460.4110.S LF
PROEJCT 1570-00-74					
0010	736+13	-	1097+72	CL	36159
0010	1098+13	-	1210+67	CL	11254
0010	1243+20	-	1251+31	CL	811
0010	1210+67	-	1251+31	BETWEEN WB DRIVE LANES	4064
0010	1220+00	-	1251+31	BETWEEN EB DRIVE LANES	3131
0010	736+13	-	801+00	EB PASSING LANE	6487
0010	815+50	-	891+00	WB PASSING LANE	7550
0010	993+00	-	1063+00	EB PASSING LANE	7000
0010	1112+00	-	1192+00	WB PASSING LANE	8000
				PROJECT 1570-00-74 CATEGORY 0010 TOTAL	84456
				CONTRACT CATEGORY 0010 TOTAL	84456

HMA PAVEMENT & TACK COAT SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	LENGTH (FT)	WIDTH (FT)	TACK COAT 455.0605 GAL	HMA PAVEMENT 4 HT 58-34 V 460.7644 TON	REMARKS
PROJECT 1570-00-74									
RURAL SEGMENT									
0010	736+13	-	744+00	LT & RT	787	62-42	532	806	DRIVE LANES, PAVED MEDIAN, EXISTING SHOULDER WIDTH
0010	744+00	-	794+00	LT & RT	5000	42	2800	4247	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	794+00	-	801+00	LT & RT	700	42-30	336	510	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	801+00	-	815+50	LT & RT	1450	30	580	880	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	815+50	-	822+50	LT & RT	700	30-42	336	510	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	822+50	-	884+00	LT & RT	6150	42	3444	5223	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	884+00	-	891+00	LT & RT	700	42-30	336	510	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	891+00	-	907+80	LT & RT	1680	30	672	1019	DRIVE LANES, EXISTING SHOULDER WIDTH,
0010	907+80	-	915+50	LT & RT	770	24	246	374	RANGE; DRIVE LANES (SEE BELOW FOR SHOULDER/TURN LANE QTY.)
0010	915+50	-	993+00	LT & RT	7750	30	3100	4702	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	993+00	-	1000+00	LT & RT	700	30-42	336	510	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1000+00	-	1056+00	LT & RT	5600	42	3136	4756	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	1056+00	-	1063+00	LT & RT	700	42-30	336	510	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1063+00	-	1096+72	LT & RT	3372	30	1349	2046	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	1096+72	-	1097+72	LT & RT	100	30	47	47	BUTTED JOINT FOR BRIDGE APPROACH, GRADE CHANGE TRANSITION
0010	1098+13	-	1099+13	LT & RT	100	30	47	47	BUTTED JOINT FOR BRIDGE APPROACH, GRADE CHANGE TRANSITION
0010	1099+13	-	1112+00	LT & RT	1287	30	515	781	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	1112+00	-	1119+50	LT & RT	750	30-42	360	546	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1119+50	-	1185+00	LT & RT	6550	42	3668	5563	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE
0010	1185+00	-	1192+00	LT & RT	700	42-30	336	510	DRIVE LANES, EXISTING SHOULDER WIDTH, PASSING LANE TAPER
0010	1192+00	-	1206+28	LT & RT	1428	30	571	866	DRIVE LANES, EXISTING SHOULDER WIDTH
0010	1206+28	-	1210+67	LT & RT	439	30-47	217	330	EXISTING PAVEMENT LIMITS
RURAL RT. TURN LANES & TAPERS BEYOND TYPICAL SHOULDERS									
0010	804+74	-	807+79	RT	305	-	39	59	80TH ST.
0010	807+79	-	814+12	RT	633	-	78	91	80TH ST.
0010	807+18	-	812+35	LT	517	-	62	89	CTH E
0010	821+42	-	828+12	RT	670	-	84	122	CE
0010	855+67	-	862+38	RT	671	-	77	111	70TH ST.
0010	859+61	-	864+91	LT	530	-	64	92	70TH ST.
0010	906+62	-	915+50	RT	888	-	130	146	RANGE SHOULDER/RT. TURN LANE
0010	907+28	-	916+61	LT	933	-	127	143	RANGE SHOULDER/RT. TURN LANE
0010	936+04	-	938+87	RT	283	-	24	31	56TH ST.
0010	973+52	-	978+73	RT	521	-	62	89	50TH ST.
0010	975+91	-	981+25	LT	534	-	63	90	50TH ST.
0010	1068+02	-	1073+96	RT	594	-	65	94	33RD ST.
0010	1069+65	-	1074+88	LT	523	-	61	87	CTH V / 33RD ST.
0010	1092+97	-	1097+72	RT	475	-	47	59	WIDENED SHOULDERS NEAR BRIDGE APPROACH
0010	1096+05	-	1097+72	LT	167	-	13	18	WIDENED SHOULDERS NEAR BRIDGE APPROACH
0010	1098+13	-	1099+80	RT	167	-	13	17	WIDENED SHOULDERS NEAR BRIDGE APPROACH
0010	1098+13	-	1099+80	LT	167	-	13	18	WIDENED SHOULDERS NEAR BRIDGE APPROACH
0010	1000+30	-	1104+50	RT	10420	-	48	68	125TH AVE.
0010	1160+62	-	1166+02	RT	540	-	68	99	15TH ST.
0010	1163+16	-	1168+25	LT	509	-	59	84	15TH ST.
PROJECT 1570-00-74 CATEGORY 0010 SUBTOTAL							24497	36900	

HMA PAVEMENT & TACK COAT SUMMARY, CONTINUED

CATEGORY	STATION	TO	STATION	LOCATION	LENGTH (FT)	WIDTH (FT)	TACK COAT 455.0605 GAL	HMA PAVEMENT 4 HT 58-34 V 460.7644 TON	REMARKS
PROJECT 1570-00-74, CONTINUED									
URBAN SEGMENT									
0010	1210+67	-	1251+31	LT & RT	4064	VAR.	3688	5594	EXIST. PAVEMENT LIMITS (EXCLUDES SIDE ROAD TONNAGE)
0010	1210+67	-	1216+61	EB MEDIAN	594	1.67	8	11	MEDIAN GUTTER PAN OVERLAY
0010	1210+67	-	1216+61	WB MEDIAN	594	1.67	8	11	MEDIAN GUTTER PAN OVERLAY
0010	1217+80	-	1225+12	WB MEDIAN	732	1.67	10	13	MEDIAN GUTTER PAN OVERLAY
0010	1223+62	-	1229+72	EB RT	610	2.00	9	13	GUTTER PAN OVERLAY
0010	1231+12	-	1237+26	WB MEDIAN	614	1.67	8	11	MEDIAN GUTTER PAN OVERLAY
0010	1238+10	-	1241+70	WB MEDIAN	360	1.67	5	7	MEDIAN GUTTER PAN OVERLAY
0010	1240+70	-	1250+46	WB RT	976	2.00	15	21	GUTTER PAN OVERLAY
0010	1216+62	-	1218+01	SIDE RD, LT	-	VAR.	25	34	CTH T
0010	1216+69	-	1217+76	SIDE RD, RT	-	VAR.	16	22	USH 63
0010	1229+71	-	1231+36	SIDE RD, RT	-	VAR.	38	53	WESTERN BLVD.
0010	1237+15	-	1238+02	SIDE RD, RT	-	VAR.	10	14	PROSSER AVE.
0010	1245+64	-	1246+79	SIDE RD, RT	-	VAR.	15	21	PROSSER AVE.
0010	1250+44	-	1251+17	SIDE RD, RT	-	VAR.	8	11	PROSSER AVE.
0010	1250+49	-	1251+31	SIDE RD, LT	-	VAR.	8	11	FRONT ST.
UNDISTRIBUTED									
0010	-		-	-	-	-	300	-	FOR USE WITH ASPHALTIC SURFACE
				PROJECT 1570-00-74 CATEGORY 0010 TOTAL			28668	42747	
PROJECT 1570-00-77									
RURAL HSIP SHOULDER WIDENING - LEFT									
0010	736+13	-	807+18	LT	7105	2	79	287	STH 46 TO CTH E
0010	812+35	-	859+61	LT	4726	2	53	191	CTH E TO 70TH ST.
0010	864+91	-	907+28	LT	4237	2	47	171	70TH ST. TO RANGE
0010	916+61	-	975+91	LT	5930	2	66	240	60TH/CTH D TO 50TH ST.
0010	981+25	-	1069+65	LT	8840	2	98	358	50TH TO 33RD ST./CTH V
0010	1074+88	-	1096+05	LT	2117	2	24	86	33RD ST./CTH V TO WEST OF BRIDGE APPROACH
0010	1099+80	-	1163+16	LT	6336	2	70	256	EAST OF BRIDGE APPROACH TO 15TH ST.
0010	1168+25	-	1215+95	LT	4770	2	53	193	15TH ST. TO CTH T
RURAL HSIP SHOULDER WIDENING - RIGHT									
0010	736+13	-	804+69	RT	6856	2	76	277	STH 46 TO 80TH ST.
0010	814+32	-	821+42	RT	710	2	8	29	80TH ST. TO CE
0010	828+12	-	855+67	RT	2755	2	31	111	CE TO 70TH ST.
0010	862+38	-	906+62	RT	4424	2	49	179	70TH ST. TO RANGE
0010	915+07	-	936+04	RT	2097	2	23	85	60TH/CTH D TO 56TH ST.
0010	938+87	-	973+52	RT	3465	2	39	140	56TH ST. TO 50TH ST.
0010	978+73	-	1068+02	RT	8929	2	99	361	50TH ST. TO 33RD ST.
0010	1073+96	-	1092+97	RT	1901	2	21	77	33RD ST. TO WEST OF BRIDGE APPROACH
0010	1099+80	-	1100+30	RT	50	2	1	2	EAST OF BRIDGE APPROACH TO 125TH AVE.
0010	1104+50	-	1160+62	RT	5612	2	62	227	125TH AVE. TO 15TH ST.
0010	1166+02	-	1211+95	RT	4593	2	51	186	15TH ST. TO USH 63
				PROJECT 1570-00-77 CATEGORY 0010 TOTAL			950	3456	
				CONTRACT CATEGORY 0010 TOTAL			29618	46203	

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ASPHALTIC SURFACE

CATEGORY	LOCATION	465.0105 TON	REMARKS
PROJECT 1570-00-74			
0010	UNDISTRIBUTED	400	FOR LEVELING AND WEDGING, PREPARING FOUNDATION FOR ASPHALTIC PAVING
PROJECT 1570-00-74 CATEGORY 0010 TOTAL		400	
CONTRACT CATEGORY 0010 TOTAL		400	

ASPHALTIC SURFACE PATCHING

CATEGORY	STATION	TO	STATION	LOCATION	465.0110 TON	REMARKS
PROJECT 1570-00-74						
0010	77793	-	77813	RT	7	PATCH FOR CULVERT REPAIR
0010	-		-	UNDISTRIBUTED	20	FOR MINOR REPAIRS
PROJECT 1570-00-74 CATEGORY 0010 TOTAL					27	
CONTRACT CATEGORY 0010 TOTAL					27	

ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES

CATEGORY	STATION	LOCATION	465.0120 TON
PROJECT 1570-00-74			
0010	736+50	RT	7
0010	790+00	RT	6
0010	844+54	LT	5
0010	892+30	LT	6
0010	903+20	RT	5
0010	904+20	LT	6
0010	917+15	LT	6
0010	928+05	RT	8
0010	950+50	LT	3
0010	965+74	LT	6
0010	1041+32	LT	4
0010	1050+90	RT	4
0010	1065+55	RT	4
0010	1182+65	LT	4
0011	1245+67	LT	1
0010	1245+17	LT	1
PROJECT 1570-00-74 CATEGORY 0010 TOTAL			76
CONTRACT CATEGORY 0010 TOTAL			76

ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL

CATEGORY	STATION	TO	STATION	LOCATION	TYPE	465.0425 LF	REMARKS
PROJECT 1570-00-77							
0010	736+13	-	1215+80	LT	1	40742	OMIT SHOULDER RUMBLES ACCORDING TO SDD & PLAN
0010	737+50	-	1211+30	RT	1	38710	OMIT SHOULDER RUMBLES ACCORDING TO SDD & PLAN
			1570-00-77 CATEGORY 0010 TOTAL			79452	
			CONTRACT CATEGORY 0010 TOTAL			79452	

ASPHALT CENTER LINE RUMBLE STRIPS 2-LANE RURAL

CATEGORY	STATION	TO	STATION	LOCATION	465.0475 LF	REMARKS
1570-00-74						
0010	74300	-	90275	CL	15175	OMIT CL RUMBLES ACCORDING TO SDD
0010	92132	-	109772	CL	16440	OMIT CL RUMBLES ACCORDING TO SDD
0010	109813	-	120628	CL	10015	OMIT CL RUMBLES ACCORDING TO SDD
			PROJECT 1570-00-74 CATEGORY 0010 TOTAL		41630	
			CONTRACT CATEGORY 0010 TOTAL		41630	

CULVERT ITEM SUMMARY

CATEGORY	STATION	LOCATION	CONCRETE COLLARS FOR PIPE 520.8000 EACH	CULVERT PIPE REINFORCED CONCRETE CLASS III 24-INCH 522.0124 LF	REMARKS
PROJECT 1570-00-74					
0010	778+03	18'-34' RT	1	16	CULVERT SEGMENT REPAIR
		PROJECT 1570-00-74 CATEGORY 0010 TOTAL	1	16	
		CONTRACT CATEGORY 0010 TOTAL	1	16	

CURB AND GUTTER SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	CONCRETE CURB & GUTTER 30-INCH TYPE D 601.0411 LF	CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D 601.0557 LF	SPECIAL(01. CONCRETE CURB AND GUTTER CURE AND SEAL TREATMENT) SPV.0090.01 LF	REMARKS
PROJECT 1570-00-74								
0010	1216+75	-	1216+95	LT	-	25	25	CTH T - NW RADIUS
0010	1217+47	-	1217+75	LT	-	32	32	CTH T - NE RADIUS
0010	1241+94	-	1242+24	LT	30	-	30	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1245+46	-	1245+86	LT	40	-	40	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1248+90	-	1249+45	LT	55	-	55	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1251+15	-	1251+20	LT	10	-	10	FRONT AVE - EAST RADIUS
PROJECT 1570-00-74 CATEGORY 0010 TOTAL					135	57	192	
CONTRACT CATEGORY 0010 TOTAL					135	57	192	

NOTE: SEE CONSTRUCTION DETIAL FOR MODIFICATIONS TO CONCRETE CURB AND GUTTER DIMENSIONS AT LOCATIONS WHERE THE GUTTER PAN WILL BE OVERLAYED WITH HMA PAVEMENT.

CATEGORY	STATION	TO	STATION	LOCATION	MGS GUARDRAIL 3 614.2300 LF	MGS THRIE BEAM TRANSITION 614.2500 LF	MGS GUARDRAIL TERMINAL EAT 614.2610 EACH	REMARKS
PROJECT 1570-00-74								
0010	1096+72	-	1097+72	LT	12.5	39.4	1	B-48-47, NW QUADRANT
0010	1098+13	-	1099+13	LT	12.5	39.4	1	B-48-47, NE QUADRANT
0010	1096+72	-	1097+72	RT	12.5	39.4	1	B-48-47, SW QUADRANT
0010	1098+13	-	1099+13	RT	12.5	39.4	1	B-48-47, SE QUADRANT
PROJECT 1570-00-74 CATEGORY 0010 TOTAL					50	157.6	4	
CONTRACT CATEGORY 0010 TOTAL					50	157.6	4	

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MOBILIZATION

CATEGORY	LOCATION	619.1000 EACH
PROJECT 1570-00-74		
0010	PROJECT 1570-00-75	0.9
	PROJECT 1570-00-74 CATEGORY 0010 TOTAL	0.9
PROJECT 1570-00-77		
0010	PROJECT 1570-00-77	0.1
	PROJECT 1570-00-77 CATEGORY 0010 TOTAL	0.1
	CONTRACT CATEGORY 0010 TOTAL	1.0

MARKERS CULVERT END

CATEGORY	STATION	LOCATION	633.5200 EACH
PROJECT 1570-00-74			
0010	863+00	RT	1
0010	1057+22	RT	1
0010	1069+52	RT	1
0010	1093+51	RT	1
0010	1215+44	RT	1
0010	1215+62	LT	1
0010	1216+54	LT	1
0010	1217+95	RT	1
0010	1240+03	LT	1
0010	1240+71	RT	1
	PROJECT 1570-00-74 CATEGORY 0010 TOTAL		10
	CONTRACT CATEGORY 0010 TOTAL		10

TRAFFIC CONTROL SUMMARY

CATEGORY	LOCATION	SIGNS/ DRUMS	DAYS	SIGN CODE	DESCRIPTION	TRAFFIC CONTROL DRUMS 643.0300 DAY	TRAFFIC CONTROL BARRICADES TYPE III 643.0420 DAY	TRAFFIC CONTROL WARNING LIGHTS TYPE C 643.0715 DAY	TRAFFIC CONTROL ARROW BOARDS 643.0800 DAY	TRAFFIC CONTROL SIGNS 643.0900 DAY	TRAFFIC CONTROL 643.5000 EACH	REMARKS
PROJECT 1570-00-74												
0010	PROJECT 1570-00-75	-	-	-	-	-	-	-	-	-	1	
0010	BEGIN PROJECT, STH 46 RAB	3	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	384	-	SEE DETAIL SHEET 'TRAFFIC CONTROL - BEGIN PROJECT
0010		2	128	M3-2	EAST	-	-	-	-	256	-	ADVANCED WARNING'
0010		2	128	M1-4	USH 8	-	-	-	-	256	-	
0010		1	128	W20-1C	ROAD WORK 1000 FT	-	-	-	-	128	-	
0010		1	128	G20-2A	END ROAD WORK	-	-	-	-	128	-	
0010		1	128	G20-1	ROAD WORK NEXT 10 MILES	-	-	-	-	128	-	
0010	STA. 736+13 - 742+74	28	113	-	TRAFFIC CONTROL DRUMS	3164	-	-	-	-	-	MEDIAN DELINEATION (IN LIEW OF TEMP. 12" DIAG.)
0010	80TH ST., CTH E	2	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	256	-	ADVANCED WARNING SIGNS FOR SIDE ROAD APPROACH
0010	70TH ST.	2	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	256	-	
0010	CTH D	2	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	256	-	
0010	56TH ST.	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128	-	
0010	50TH ST.	2	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	256	-	
0010	33RD ST. CTH V	2	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	256	-	
0010	125TH AVE.	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128	-	
0010	15TH ST.	2	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	256	-	
0010	CTH T	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128	-	
0010	WESTERN BLVD.	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128	-	
0010	ANTIQUE RD.	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128		
0010	OSTERMAN DR.	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128	-	
0010	FRONT AVE.	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128	-	
0010	USH 63 SOUTH INT.	1	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	128	-	ADVANCED WARNING FOR INTERSECING USH 63
0010		1	128	W20-1C	ROAD WORK 1000 FT	-	-	-	-	128	-	
0010		1	128	W20-1D	ROAD WORK 500 FT	-	-	-	-	128	-	
0010		1	128	G20-2A	END ROAD WORK	-	-	-	-	128	-	
0010	USH 8 NEAR STA. 1206+00	1	128	G20-1	ROAD WORK NEXT 9 MILES	-	-	-	-	128	-	FOR WESTBOUND TRAFFIC, WEST OF USH 63 INT.
0010	END PROJECT, CTH K RAB	3	128	W20-1A	ROAD WORK AHEAD	-	-	-	-	384	-	SEE DETAIL SHEET 'TRAFFIC CONTROL - END PROJECT
0010		2	128	M3-4	WEST	-	-	-	-	256	-	ADVANCED WARNING AND SINGLE LANE CLOSURE'
0010		2	128	M1-4	USH 8	-	-	-	-	256	-	
0010		1	128	W20-1B	ROAD WORK 1500 FT	-	-	-	-	128	-	
0010		1	121	W20-1C	ROAD WORK 1000 FT	-	-	-	-	121	-	
0010		1	7	W20-5A	RIGHT LANE AHEAD	-	-	-	-	7	-	
0010		1	128	G20-1	ROAD WORK NEXT 10 MILES	-	-	-	-	128	-	
0010		1	7	W4-2R	RIGHT LANE ENDS	-	-	-	-	7	-	
0010		2	7	R3-20R	BEGIN RIGHT TURN LANE	-	-	-	-	14	-	
0010		1	7	-	ARROW BOARD	-	-	-	7	-	-	
0010		2	7	R11-2L	BARRICADE WITH SIGN, LN CLOSED	-	14	-	-	14	-	
0010		60	7	-	TRAFFIC CONTROL DRUMS	420	-	70	-	-	-	10 DRUMS WITH LIGHTS AT TAPER
0010		1	128	G20-2A	END ROAD WORK	-	-	-	-	128	-	
0010	UNDISTRIBUTED	-	-	W08-1	BUMP	-	-	-	-	600	-	SDD 15D44, SIGNING ON MILLED SURFACES AND
0010		-	-	W016-7L	DIAGONAL DOWN ARROW	-	-	-	-	600	-	TRAFFIC CONTROL SIGNING FOR UNEVEN SURFACES
0010		-	-	W8-11	UNEVEN LANES	-	-	-	-	600	-	
0010		-	-	R4-1	DO NOT PASS	-	-	-	-	600	-	
0010		-	-	W08-52	GROOVED PAVEMENT	-	-	-	-	600	-	
0010		-	-	W013-1	45 M.P.H.	-	-	-	-	600	-	
					PROJECT 1570-00-74 CATEGORY 0010 TOTAL	3584	14	70	7	9395	1	
					CONTRACT CATEGORY 0010 TOTAL	3584	14	70	7	9395	1	

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PAVEMENT MARKING SUMMARY

PROJECT 1570-00-74

CATEGORY	STATION	TO	STATION	LOCATION	MARKING LINE EPOXY 4-INCH 646.1020 LF	MARKING LINE GROOVED WET REF EPOXY 4-INCH 646.1040 LF	MARKING LINE GROOVED WET REF EPOXY 8-INCH 646.3040 LF	MARKING LINE SAME DAY EPOXY 4-INCH 646.4520 LF	MARKING ARROW EPOXY 646.5020 EACH	MARKING WORD EPOXY 646.5120 EACH	MARKING STOP LINE EPOXY 18-INCH 646.6120 LF	MARKING DIAGONAL EPOXY 12-INCH 646.7120 LF	MARKING CORRUGATED MEDIAN EPOXY 646.8020 SF	MARKING CURB EPOXY 646.8120 LF	MARKING ISLAND NOSE EPOXY 646.8220 EACH	REMARKS
EDGE LINES AND CENTERLINES																
0010	736+13		1240+70	EDGE LINES LT	-	50457	-	-	-	-	-	-	-	-	-	SOLID, WHITE
0010	736+13		1245+67	EDGE LINES RT	-	50954	-	-	-	-	-	-	-	-	-	SOLID, WHITE
0010	736+13	-	757+27	CL	4228	-	-	4228	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	757+27	-	770+00	CL	1592	-	-	1592	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	770+00	-	783+60	CL	2720	-	-	2720	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	783+60	-	801+00	CL	2175	-	-	2175	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	801+00	-	808+87	CL	197	-	-	197	-	-	-	-	-	-	-	SKIP, YELLOW*
0010	808+87	-	815+50	CL	829	-	-	829	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	815+50	-	817+78	CL	456	-	-	456	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	817+78	-	863+15	CL	5672	-	-	5672	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	863+15	-	877+40	CL	2850	-	-	2850	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	877+40	-	895+46	CL	2258	-	-	2258	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	895+46	-	902+75	CL	183	-	-	183	-	-	-	-	-	-	-	SKIP, YELLOW*
0010	902+75	-	921+32	CL	-	-	-	465	-	-	-	-	-	-	-	SKIP, YELLOW #
0010	921+32	-	943+42	CL	553	-	-	553	-	-	-	-	-	-	-	SKIP, YELLOW*
0010	943+42	-	955+10	CL	1460	-	-	1460	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	955+10	-	957+90	CL	560	-	-	560	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	957+90	-	969+00	CL	1388	-	-	1388	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	969+00	-	982+00	CL	325	-	-	325	-	-	-	-	-	-	-	SKIP, YELLOW*
0010	982+00	-	993+00	CL	1375	-	-	1375	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	993+00	-	1015+84	CL	4568	-	-	4568	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	1015+84	-	1078+60	CL	7845	-	-	7845	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	1078+60	-	1107+05	CL	712	-	-	712	-	-	-	-	-	-	-	SKIP, YELLOW*
0010	1107+05	-	1122+90	CL	1982	-	-	1982	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	1122+90	-	1134+15	CL	2250	-	-	2250	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	1134+15	-	1159+62	CL	3184	-	-	3184	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	1159+62	-	1183+50	CL	4776	-	-	4776	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	1183+50	-	1196+40	CL	1613	-	-	1613	-	-	-	-	-	-	-	SOLID-SKIP, YELLOW*
0010	1196+40	-	1206+28	CL	1976	-	-	1976	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW*
0010	1206+28	-	1251+31	CL	-	-	-	9006	-	-	-	-	-	-	-	DOUBLE SOLID, YELLOW ^
	PROJECT 1570-00-74 CATEGORY 0010 SUBTOTAL				57727	101411	0	67198	0	0	0	0	0	0	0	

NOTES: *THE 'MARKING LINE SAME DAY EPOXY 4-INCH' IN LOCATIONS WHERE CENTERLINE RUMBLE STRIPS WILL BE CONSTRUCTED SHALL BE PLACED AT A REDUCED APPLICATION RATE; SEE SPECIAL PROVISIONS.

RANGE, NO CL RUMBLE STRIP

^ URBAN, NO CL RUMBLE STRIP

PAVEMENT MARKING SUMMARY, CONTINUED

PROJECT 1570-00-74

CATEGORY	STATION	TO	STATION	LOCATION	MARKING LINE EPOXY 4-INCH 646.1020 LF	MARKING LINE GROOVED WET REF EPOXY 4-INCH 646.1040 LF	MARKING LINE GROOVED WET REF EPOXY 8-INCH 646.3040 LF	MARKING LINE SAME DAY EPOXY 4-INCH 646.4520 LF	MARKING ARROW EPOXY (TYPE 2) 646.5020 EACH	MARKING WORD EPOXY ("ONLY") 646.5120 EACH	MARKING STOP LINE EPOXY 18-INCH 646.6120 LF	MARKING DIAGONAL EPOXY 12-INCH 646.7120 LF	MARKING CORRUGATED MEDIAN EPOXY 646.8020 SF	MARKING CURB EPOXY 646.8120 LF	MARKING ISLAND NOSE EPOXY 646.8220 EACH	REMARKS
LANE LINES, MEDIANS, TURN LANES & SIDEROADS																
0010	736+13	-	742+93	MEDIAN	-	-	-	1360	-	-	-	260	-	-	-	SOLID, YELLOW
0010	736+13	-	791+55	EB PASSING LANE LINE	-	-	-	1386	-	-	-	-	-	-	-	SKIP, WHITE
0010	824+25	-	891+00	WB PASSING LANE LINE	-	-	-	1669	-	-	-	-	-	-	-	SKIP, WHITE
0010	993+00	-	1050+00	EB PASSING LANE LINE	-	-	-	1425	-	-	-	-	-	-	-	SKIP, WHITE
0010	1124+40	-	1192+00	WB PASSING LANE LINE	-	-	-	1690	-	-	-	-	-	-	-	SKIP, WHITE
0010	1206+28	-	1210+67	MEDIAN	-	-	-	710	-	-	-	85	351	20	1	SOLID, YELLOW
0010	1217+80	-	1225+11	MEDIAN	-	-	-	-	-	-	-	-	-	75	-	SOLID, YELLOW
0010	1220+00		1223+22	USH 63 LT LANE LINE	-	-	-	322	-	-	-	-	-	-	-	SOLID, WHITE
0010	1225+58	-	1230+07	MEDIAN	-	-	-	-	-	-	-	-	-	110	-	SOLID, YELLOW
0010	1231+12	-	1237+25	MEDIAN	-	-	-	-	-	-	-	-	-	75	-	SOLID, YELLOW
0010	1223+22	-	1251+31	EB LANE LINE	-	-	-	703	-	-	-	-	-	-	-	SKIP, WHITE
0010	1214+90	-	1251+31	WB LANE LINE	-	-	-	911	-	-	-	-	-	-	-	SKIP, WHITE
0010	1238+10	-	1246+02	MEDIAN	-	-	-	564	-	-	-	106	485	20	1	SOLID, YELLOW
0010	805+82	-	807+80	EB RT TURN LANE	-	-	198	-	2	1	-	-	-	-	-	SOLID, WHITE (80TH ST)
0010	809+28	-	811+22	WB RT TURN LANE	-	-	194	-	2	1	-	-	-	-	-	SOLID, WHITE (CTH E)
0010	822+55	-	826+05	EB RT TURN LANE	-	-	350	-	2	1	-	-	-	-	-	SOLID, WHITE (CE)
0010	856+80	-	860+33	EB RT TURN LANE	-	-	353	-	2	1	-	-	-	-	-	SOLID, WHITE (70TH ST)
0010	861+77	-	863+78	WB RT TURN LANE	-	-	201	-	2	1	-	-	-	-	-	SOLID, WHITE (70TH ST)
0010	907+75	-	912+94	EB RT TURN LANE	-	-	519	-	2	1	-	-	-	-	-	SOLID, WHITE (CTH D / 60TH
0010	914+42	-	915+48	WB RT TURN LANE	-	-	106	-	2	-	-	-	-	-	-	SOLID, WHITE (CTH D / 60TH
0010	974+65	-	976+64	EB RT TURN LANE	-	30	199	-	2	1	-	-	-	-	-	SOLID, WHITE (50TH ST)
0010	978+08	-	980+12	WB RT TURN LANE	-	30	204	-	2	1	-	-	-	-	-	SOLID, WHITE (50TH ST)
0010	1069+15	-	1070+35	EB RT TURN LANE	-	-	120	-	2	-	-	-	-	-	-	SOLID, WHITE (33RD ST)
0010	1071+60	-	1073+75	WB RT TURN LANE	-	-	215	-	2	1	-	-	-	-	-	SOLID, WHITE (CTH V / 33RD
0010	1101+43	-	1102+42	EB RT TURN LANE	-	-	99	-	2	-	-	-	-	-	-	SOLID, WHITE (125TH AVE)
0010	1161+75	-	1163+82	WB RT TURN LANE	-	-	207	-	2	1	-	-	-	-	-	SOLID, WHITE (15TH ST)
0010	1165+11	-	1167+12	EB RT TURN LANE	-	-	201	-	2	1	-	-	-	-	-	SOLID, WHITE (15TH ST)
0010	1213+08	-	1216+57	EB RT TURN LANE	-	-	349	-	2	1	-	-	-	-	-	SOLID, WHITE (USH 63)
0010	1214+71	-	1216+57	EB LT TURN LANE	-	-	186	-	2	1	-	-	-	-	-	SOLID, WHITE (CTH T)
0010	1217+84	-	1222+41	WB LT TURN LANE	-	-	457	-	2	1	-	-	-	-	-	SOLID, WHITE (USH 63)
0010	1226+98	-	1229+72	EB RT TURN LANE	-	-	274	-	2	1	-	-	-	-	-	SOLID, WHITE (WESTERN BLVD
0010	1231+17	-	1232+23	WB LT TURN LANE	-	-	106	-	2	-	-	-	-	-	-	SOLID, WHITE (WESTERN BLVD
0010	1235+19	-	1237+18	EB RT TURN LANE	-	-	199	-	2	1	-	-	-	-	-	SOLID, WHITE (PROSSER AVE)
0010	1238+14	-	1239+17	WB LT TURN LANE	-	-	103	-	2	-	-	-	-	-	-	SOLID, WHITE (PROSSER AVE)
0010	1243+41	-	1245+66	EB RT TURN LANE	-	-	225	-	2	1	-	-	-	-	-	SOLID, WHITE (PROSSER AVE)
0010	1216+96		1217+19	SIDEROAD LT	12	-	-	60	-	-	23	-	-	-	-	CTH T
0010	1217+33		1217+53	SIDEROAD RT	12	-	-	40	-	-	20	-	-	-	-	USH 63
0010	1230+76	-	1231+11	SIDEROAD RT	-	-	-	96	-	-	35	-	-	-	-	WESTERN BLVD
0010	1237+60	-	1237+85	SIDEROAD RT	-	-	-	-	-	-	25	-	-	-	-	PROSSER AVE
0010	1246+29	-	1246+64	SIDEROAD RT	-	-	-	-	-	-	35	-	-	-	-	PROSSER AVE
0010	1250+79	-	1251+01	SIDEROAD LT	-	-	-	-	-	-	22	-	-	-	-	PROSSER AVE
0010	1250+80	-	1250+99	SIDEROAD RT	-	-	-	-	-	-	19	-	-	-	-	FRONT AVE
PROJECT 1570-00-74 CATEGORY 0010 SUBTOTAL					24	60	5065	10936	44	17	179	451	836	300	2	
PROJECT 1570-00-74 CATEGORY 0010 TOTAL					57751	101471	5065	78134	44	17	179	451	836	300	2	
CONTRACT CATEGORY 0010 TOTAL					57751	101471	5065	78134	44	17	179	451	836	300	2	

3

MARKING REMOVAL LINE 4-INCH

CATEGORY	STATION	TO	STATION	LOCATION	646.9000 LF	REMARKS
PROJECT 1570-00-74						
0010	1252+25	-	1254+75	WB LANE LINE DASHES	75	FOR TAPER OF SINGLE LANE CLOSURE
				PROJECT 1570-00-74 CATEGORY 0010 TOTAL	75	
				CONTRACT CATEGORY 0010 TOTAL	75	

TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH

CATEGORY	STATION	TO	STATION	LOCATION	649.0150 LF	REMARKS
PROJECT 1570-00-74						
0010	1249+55	-	1254+20	WB LANE CLOSURE TAPER	450	SOLID, WHITE (SEE END PROJECT TC SHEET)
				PROJECT 1570-00-74 CATEGORY 0010 TOTAL		
				CONTRACT CATEGORY 0010 TOTAL	450	

TEMPORARY MARKING LINE PAINT 4-INCH

CATEGORY	STATION	TO	STATION	LENGTH (FT)	APPLICATIONS*	MULTIPLIER	LOCATION	649.0105 LF	REMARKS
PROJECT 1570-00-74									
0010	736+13	-	757+27	2114	2	2	CL	8456	DOUBLE SOLID, YELLOW
0010	757+27	-	770+00	1273	2	1.08	CL	2750	SOLID-SKIP, YELLOW
0010	770+00	-	783+60	1360	2	2	CL	5440	DOUBLE SOLID, YELLOW
0010	783+60	-	801+00	1740	2	1.08	CL	3759	SOLID-SKIP, YELLOW
0010	801+00	-	808+87	787	2	0.08	CL	126	SKIP, YELLOW
0010	808+87	-	815+50	663	2	1.08	CL	1433	SOLID-SKIP, YELLOW
0010	815+50	-	817+78	228	2	2	CL	912	DOUBLE SOLID, YELLOW
0010	817+78	-	863+15	4537	2	1.08	CL	9800	SOLID-SKIP, YELLOW
0010	863+15	-	877+40	1425	2	2	CL	5700	DOUBLE SOLID, YELLOW
0010	877+40	-	895+46	1806	2	1.08	CL	3901	SOLID-SKIP, YELLOW
0010	895+46	-	902+75	729	2	0.08	CL	117	SKIP, YELLOW
0010	902+75	-	921+32	1857	2	0.08	CL	298	SKIP, YELLOW
0010	921+32	-	943+42	2210	2	0.08	CL	354	SKIP, YELLOW
0010	943+42	-	955+10	1168	2	1.08	CL	2523	SOLID-SKIP,
0010	955+10	-	957+90	280	2	2	CL	1120	DOUBLE SOLID,
0010	957+90	-	969+00	1110	2	1.08	CL	2398	SOLID-SKIP,
0010	969+00	-	982+00	1300	2	0.08	CL	208	SKIP, YELLOW
0010	982+00	-	993+00	1100	2	1.08	CL	2376	SOLID-SKIP, YELLOW
0010	993+00	-	1015+84	2284	2	2	CL	9136	DOUBLE SOLID, YELLOW
0010	1015+84	-	1078+60	6276	2	1.08	CL	13557	SOLID-SKIP, YELLOW
0010	1078+60	-	1107+05	2845	2	0.08	CL	456	SKIP, YELLOW
0010	1107+05	-	1122+90	1585	2	1.08	CL	3424	SOLID-SKIP, YELLOW
0010	1122+90	-	1134+15	1125	2	2	CL	4500	DOUBLE SOLID, YELLOW
0010	1134+15	-	1159+62	2547	2	1.08	CL	5502	SOLID-SKIP, YELLOW
0010	1159+62	-	1183+50	2388	2	2	CL	9552	DOUBLE SOLID, YELLOW
0010	1183+50	-	1196+40	1290	2	1.08	CL	2787	SOLID-SKIP, YELLOW
0010	1196+40	-	1206+28	988	2	2	CL	3952	DOUBLE SOLID, YELLOW
0010	1206+28	-	1251+31	4503	2	2	CL	18012	DOUBLE SOLID, YELLOW
0010	736+13	-	742+74	661	2	1	MEDIAN	1322	SOLID, YELLOW
0010	736+13	-	791+55	5542	2	0.08	EB PASSING LANE LINE	887	SKIP, WHITE
0010	824+25	-	891+00	6675	2	0.08	WB PASSING LANE LINE	1068	SKIP, WHITE
0010	993+00	-	1050+00	5700	2	0.08	EB PASSING LANE LINE	912	SKIP, WHITE
0010	1124+40	-	1192+00	6760	2	0.08	WB PASSING LANE LINE	1082	SKIP, WHITE
0010	1207+10	-	1210+67	357	2	1	MEDIAN	714	SOLID, YELLOW
0010	1220+00		1223+22	322	2	1	USH 63 LANE LINES	644	SOLID, WHITE
0010	1223+22	-	1251+31	2809	2	0.08	EB LANE LINE	450	SKIP, WHITE
0010	1214+90	-	1251+31	3641	2	0.08	WB LANE LINE	583	SKIP, WHITE
0010	1243+20	-	1245+00	180	2	1	MEDIAN	360	SOLID, YELLOW
							PROJECT 1570-00-74 CATEGORY 0010 TOTAL	130571	
							CONTRACT CATEGORY 0010 TOTAL	130571	

NOTES: *QUANTITIES ASSUME ONE APPLICATION ON MILLED SURFACE AND SECOND APPLICATION ON HMA LOWER LAYER.

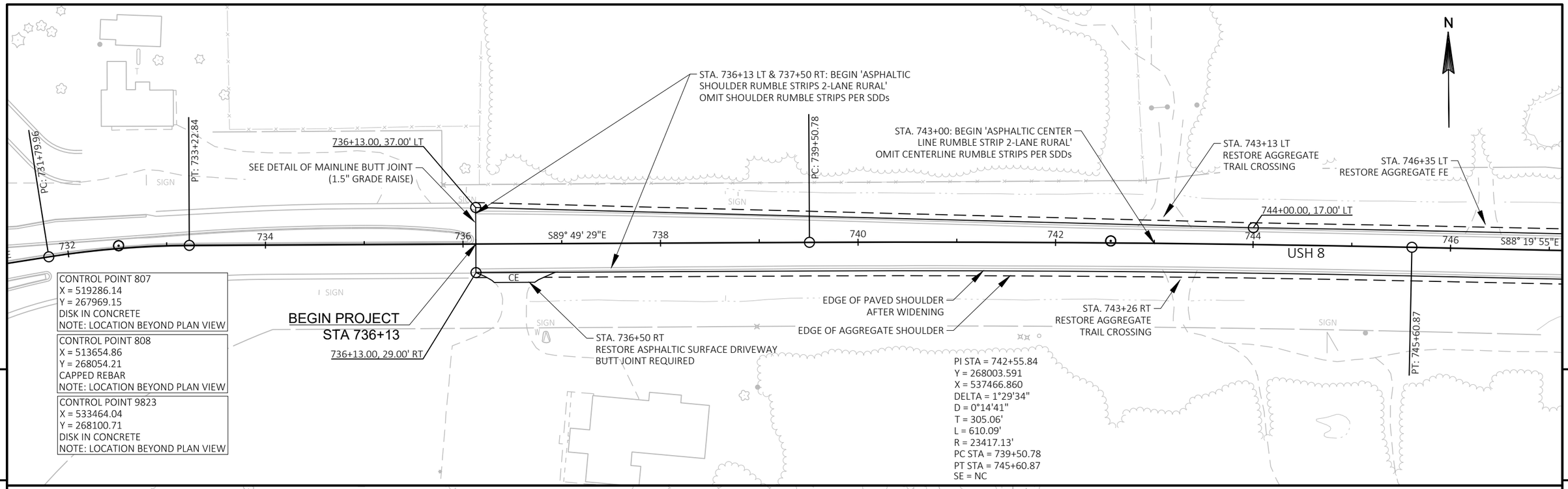
ALSO SEE PAVEMENT MARKING SUMMARY TABLE FOR LOCATIONS WHERE 'MARKING LINE SAME DAY EPOXY 4-INCH' WILL BE MARKED AT A REDUCED APPLICATION RATE PRIOR TO CENTERLINE RUMBLE STRIP INSTALLATION; SEE SPECIAL PROVISIONS.

CONSTRUCITON STAKING SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER 650.5500 LF	CONSTRUCTION STAKING RESURFACING REFERENCE 650.8000 LF	REMARKS
PROJECT 1570-00-74							
0010	736+13	-	1097+72	CL	-	36159	
0010	1098+13	-	1251+31	CL	-	15318	
0010	1216+75	-	1216+95	LT	25	-	CTH T - NW RADIUS
0010	1217+47	-	1217+75	LT	32	-	CTH T - NE RADIUS
0010	1241+94	-	1242+24	LT	30	-	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1245+46	-	1245+86	LT	40	-	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1248+90	-	1249+45	LT	55	-	DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1251+15	-	1251+20	LT	10	-	FRONT AVE - EAST RADIUS
			PROJECT 1570-00-74 CATEGORY 0010 TOTAL		192	51477	
			CONTRACT CATEGORY 0010 TOTAL		192	51477	

SAWING SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	SAWING ASPHALT 690.0150 LF	SAWING CONCRETE 690.0250 LF	REMARKS
PROJECT 1570-00-74							
0010	777+93	-	778+13	RT	50	-	AT CULVERT REPAIR
0010	1216+75	-	1217+75	LT	61	10	FOR CURB & GUTTER REPLACEMENT - CTH T SIDEROAD
0010	1241+94	-	1242+24	LT	34	5	FOR DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1245+46	-	1245+86	LT	65	5	FOR DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1248+90	-	1249+45	LT	100	5	FOR DRIVEWAY CURB & GUTTER REPLACEMENT
0010	1251+15	-	1251+20	LT	15	5	FOR CURB & GUTTER REPLACEMENT - FRONT AVE, EAST RADIUS
			PROJECT 1570-00-74 CATEGORY 0010 TOTAL		325	30	
			CONTRACT CATEGORY 0010 TOTAL		325	30	



PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 50	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-1

PLOT DATE : 10/8/2019 4:49 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

WISDOT/CADDS SHEET 44

N

SIGN

STA. 775+40 LT
RESTORE AGGREGATE CE

762 764 766 768 S89° 53' 14"E 770 772 774 776

USH 8

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

STA. 772+54 RT
RESTORE AGGREGATE FE

5

5

RUINS

STA. 778+03
EXISTING 120' X 24" CLASS III RCP

STA. 782+26 LT
RESTORE AGGREGATE FE

FE

SIGN

778 780 782 S89° 53' 14"E 784 786 788 790 792

USH 8

38'

SAWCUT REQUIRED

FO FO FO FO

SPECTRUM

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

PE

STA. 790+00
RESTORE ASPHALTIC SURFACE DRIVEWAY
BUTT JOINT REQUIRED

UTILITIES SHOWN ON THIS LAYOUT ARE
FROM FIELD SURVEY AFTER LOCATE

REMOVE AND REPLACE TWO 8' SECTIONS OF CULVERT PIPE
REINFORCED CONCRETE CLASS III 24-INCH;
SEE SDD 'JOINT TIES FOR CONCRETE PIPE AND CONCRETE PIPE COLLARS'.

IF AN OVERNIGHT, EASTBOUND OUTSIDE LANE CLOSURE IS REQUIRED,
SEE SDD 'TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON FREEWAY/EXPRESSWAY'.

N

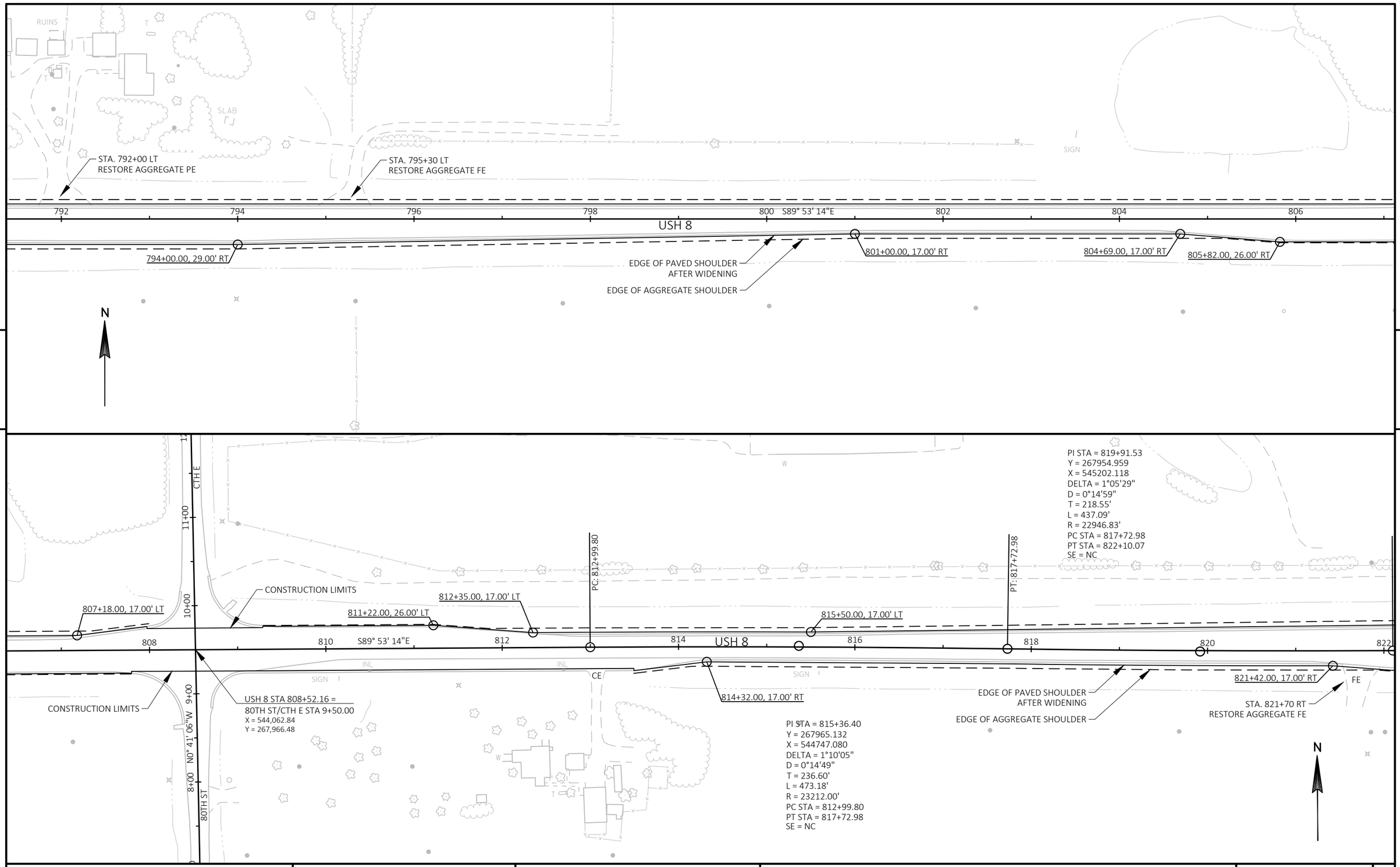
SEPC

SEPC

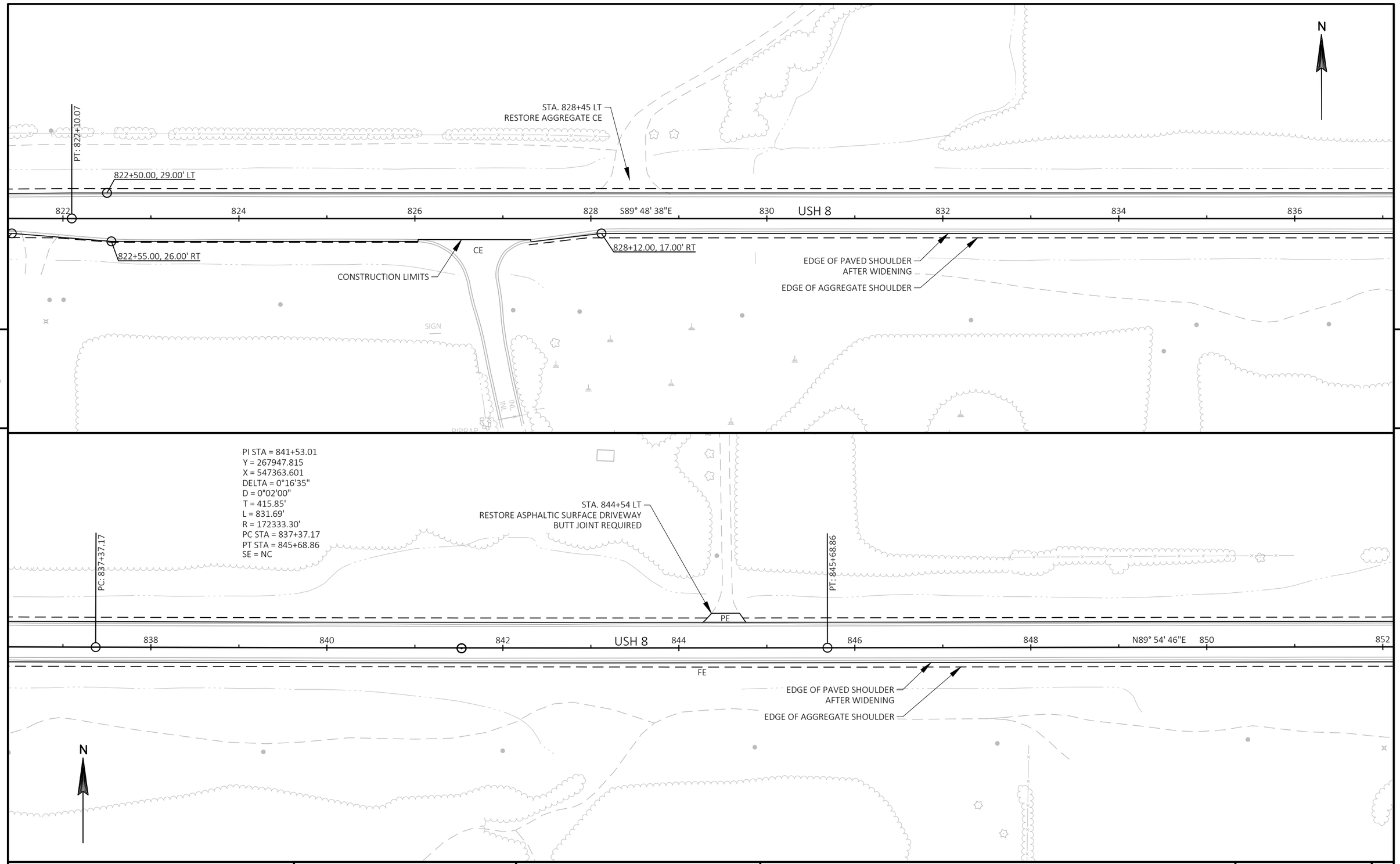
PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 51	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-2

PLOT DATE : 10/8/2019 4:50 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########



PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 52	E



5

5

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 53	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-4

PLOT DATE : 10/8/2019 4:51 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

N

USH 8 STA 861+05.15 =
70TH ST STA 9+50.00
X = 549,315.73
Y = 267,950.78

859+61.00, 29.00' LT

CONSTRUCTION LIMITS

863+78.00, 38.00' LT

864+91.00, 29.00' LT

70TH ST

11+00

10+00

9+00

8+00

N0° 16' 07"E

852 854 856 USH 8 858 N89° 54' 46"E 860 862 864 866

855+67.00, 17.00' RT

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

862+38.00, 17.00' RT

CONSTRUCTION LIMITS

MARKERS CULVERT END

RIPRAP

SLAB

SLAB
SUBSTATION

INL

FLAG

W

STA. 872+67 LT
RESTORE AGGREGATE PE

STA. 878+70 LT
RESTORE AGGREGATE PE

868 870 872 N89° 54' 46"E 874 USH 8 876 878 880 882

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

STA. 874+00 RT
RESTORE AGGREGATE PE

878+71.36, 82.86' RT

CONTROL POINT 804
X = 551082.07
Y = 267870.61
CAPPED REBAR

N

5

5

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 54 E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-5

PLOT DATE : 10/8/2019 4:51 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

PI STA = 888+42.55
Y = 267954.947
X = 552053.134
DELTA = 2°23'38"
D = 0°15'01"
T = 478.29'
L = 956.44'
R = 22892.23'
PC STA = 883+64.26
PT STA = 893+20.70
SE = NC

PC: 883+64.26

884+00.00, 29.00' LT

STA. 885+00 LT
RESTORE AGGREGATE PE

STA. 888+32 LT
RESTORE AGGREGATE PE

STA. 892+30 LT
RESTORE ASPHALTIC SURFACE DRIVEWAY
BUTT JOINT REQUIRED

891+00.00, 17.00' LT

PE

PT: 893+20.70

STA. 895+30 LT
RESTORE AGGREGATE PE

882 N89° 54' 46"E 884 886 USH 8 888 890 892 894 896

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

STA. 890+50 LT
RESTORE AGGREGATE PE

N

5

5

N

STA. 899+80 LT
RESTORE AGGREGATE PE

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER
AFTER WIDENING

STA. 901+65 & 902+06 LT
RESTORE AGGREGATE PE

PT: 902+76.55

STA. 904+20 LT
RESTORE ASPHALTIC SURFACE DRIVEWAY
BUTT JOINT REQUIRED

907+28.00, 17.00' LT

PE

898 900 902 PE 904 N89° 51' 43"E 906 908 910 912 USH 8

PI STA = 897+98.69
Y = 267996.342
X = 553008.518
DELTA = 2°20'34"
D = 0°14'42"
T = 477.99'
L = 955.85'
R = 23375.57'
PC STA = 893+20.70
PT STA = 902+76.55
SE = NC

OMIT 'ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL' AND
'ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL'
WITHIN THE COMMUNITY OF RANGE (STA. 902+75 TO 921+32)

STA. 903+20
RESTORE ASPHALTIC SURFACE DRIVEWAY
BUTT JOINT REQUIRED

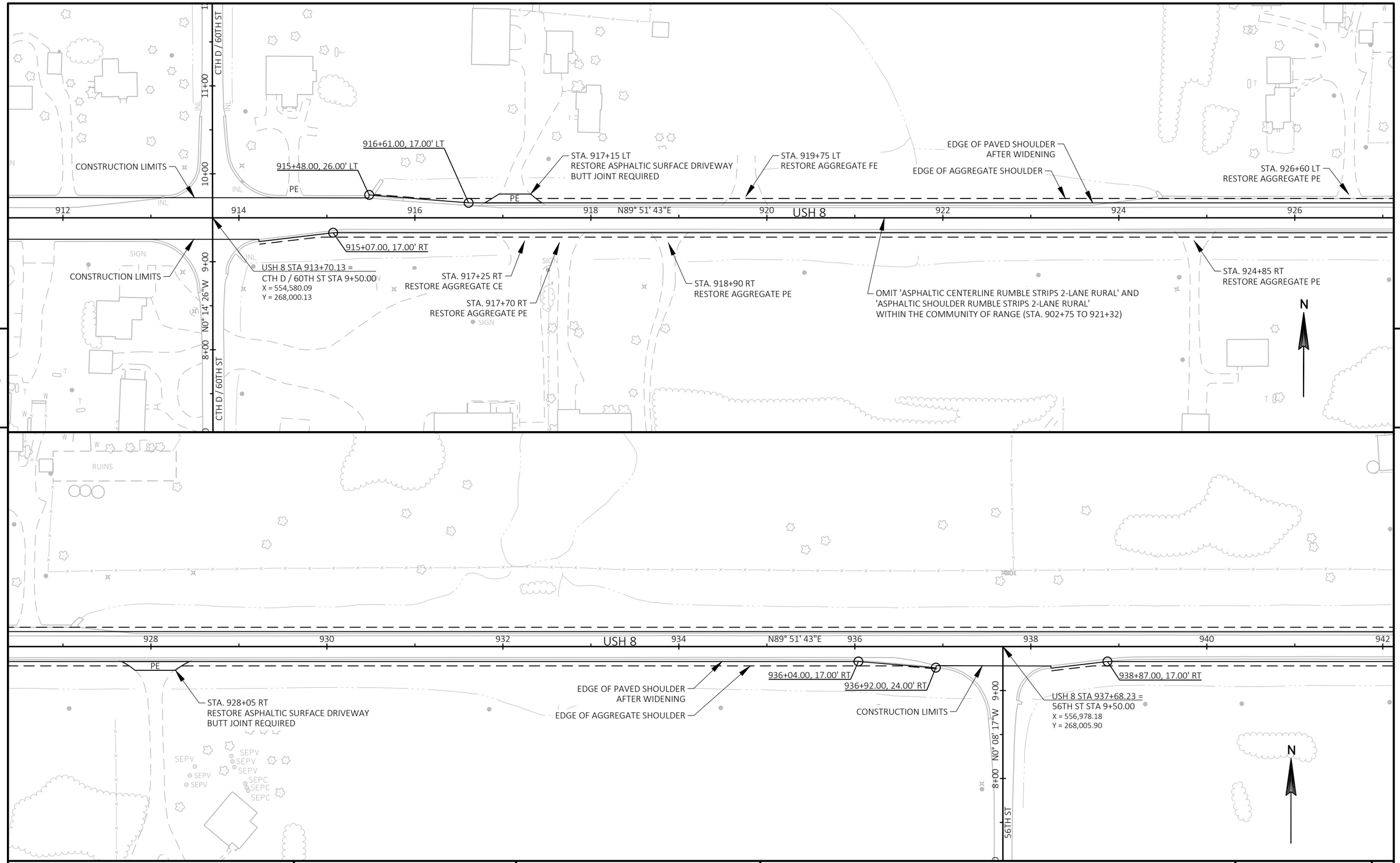
STA. 904+80 RT
RESTORE AGGREGATE PE

STA. 905+90 RT
RESTORE AGGREGATE PE

906+62.00, 17.00' RT

STA. 906+75 RT
RESTORE AGGREGATE PE

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 55	E



5

5

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 56	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-7

PLOT DATE : 10/8/2019 4:53 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

WISDOT/CADDS SHEET 44

PI STA = 944+20.83
Y = 268007.480
X = 557630.771
DELTA = 0°31'41"
D = 0°15'46"
T = 100.50'
L = 201.00'
R = 21804.78'
PC STA = 943+20.32
PT STA = 945+21.33
SE = NC

N

STA. 942+80 LT
RESTORE AGGREGATE PE

PC: 943+20.32

PT: 945+21.33

STA. 950+50 LT
RESTORE ASPHALTIC SURFACE DRIVEWAY
BUTT JOINT REQUIRED

PE

942 N89° 51' 43"E 944 946 USH 8 948 N89° 20' 02"E 950 952 954 956

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

5

5

N

N

SEPV
SEPC

STA. 957+20 LT
RESTORE AGGREGATE FE

STA. 965+74 LT
RESTORE ASPHALTIC SURFACE DRIVEWAY
BUTT JOINT REQUIRED

SIGN

PC: 971+97.83

PE

958 960 962 964 N89° 20' 02"E 966 USH 8 968 970 972

N89° 20' 02"E

STA. 957+40
RESTORE AGGREGATE PE

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 57	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-8

PLOT DATE : 10/8/2019 4:53 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ############

PI STA = 973+94.67
Y = 268042.059
X = 560604.417
DELTA = 0°59'03"
D = 0°15'00"
T = 196.84'
L = 393.66'
R = 22918.31'
PC STA = 971+97.83
PT STA = 975+91.50
SE = NC

975+96.00, 173.00' LT

PC: 971+97.83

PT: 975+91.50

50TH ST

11+00

10+00

9+00

8+00

STA. 972+35 LT
RESTORE AGGREGATE PE

CONSTRUCTION LIMITS

975+91.00, 17.00' LT

980+12.00, 26.00' LT

981+25.00, 17.00' LT

972 974 976 978 USH 8 980 982 984 986

N89° 20' 02"E

N88° 20' 59"E

973+52.00, 17.00' RT

974+65.00, 26.00' RT

978+73.00, 17.00' RT

CONSTRUCTION LIMITS

USH 8 STA 977+34.39 =
50TH ST STA 9+50.00
X = 560,944.00
Y = 268,051.85

N0° 11' 47"E

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

N

5

5

PC: 990+66.65

PT: 996+26.93

STA. 988+00 LT
RESTORE AGGREGATE PE

STA. 992+20 LT
RESTORE AGGREGATE FE

CONTROL POINT 802
X = 563184.88
Y = 268164.72
DISK IN CONCRETE

999+76.19, 63.70' LT

N88° 20' 59"E 988 990 992 994 996 998 N89° 45' 01"E 1000 1002

USH 8

993+00.00, 17.00' RT

1000+00.00, 29.00' RT

STA. 992+20 RT
RESTORE AGGREGATE PE

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

PI STA = 993+46.80
Y = 268098.282
X = 562555.749
DELTA = 1°24'02"
D = 0°15'00"
T = 280.15'
L = 560.27'
R = 22918.31'
PC STA = 990+66.65
PT STA = 996+26.93
SE = NC

N

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 58	E

N

1002 1004 1006 1008 N89° 45' 01"E 1010 1012 1014 1016

USH 8

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

5

5

PC: 1021+01.01

STA. 1022+00 LT
RESTORE AGGREGATE PE

PT: 1023+41.20

1018 N89° 45' 01"E 1020 1022 1024 1026 1028 S89° 43' 23"E 1030 1032

USH 8

STA. 1018+00 RT
RESTORE AGGREGATE FE

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

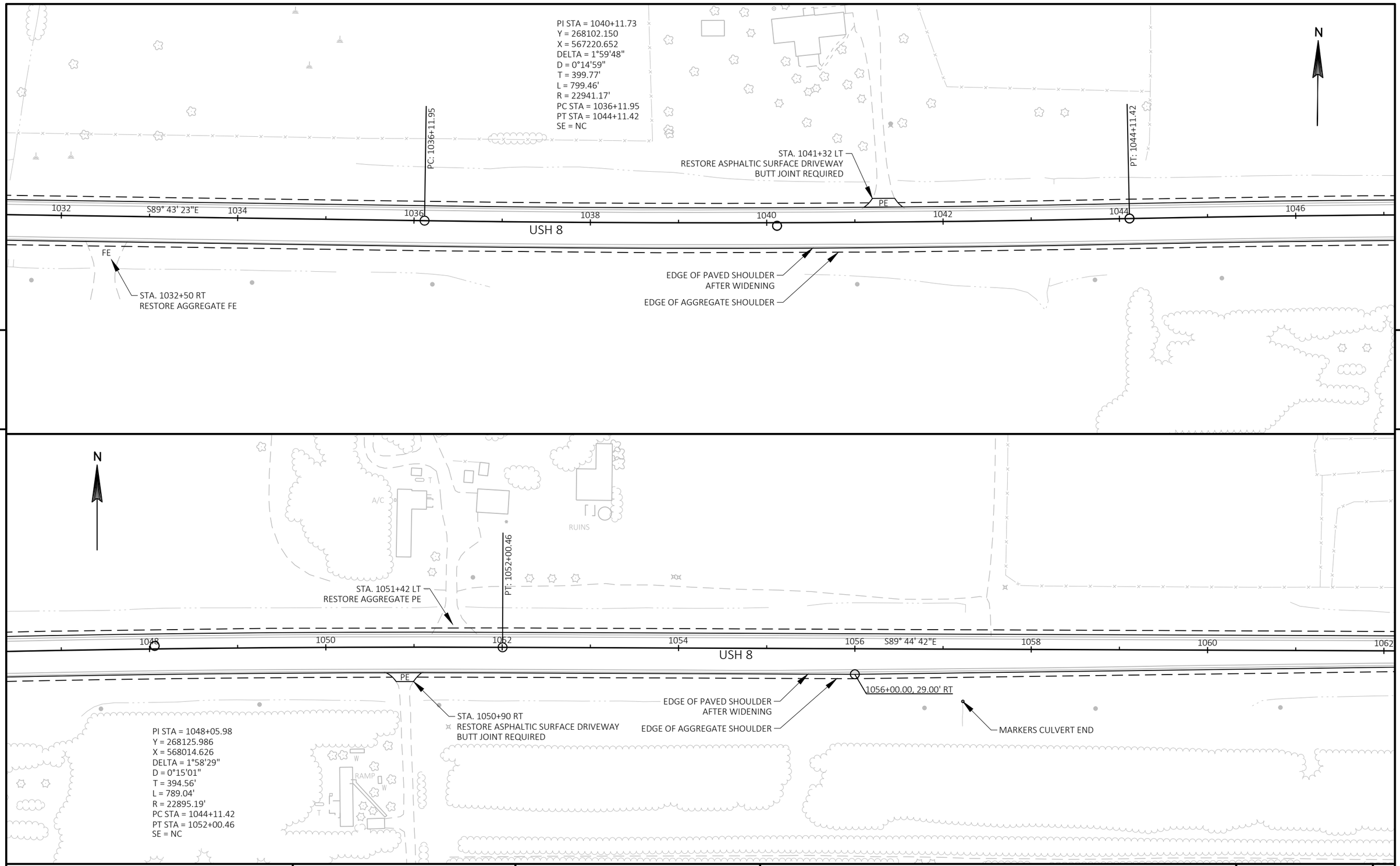
N

PI STA = 1022+21.11
Y = 268110.809
X = 565430.055
DELTA = 0°31'36"
D = 0°13'10"
T = 120.10'
L = 240.19'
R = 26124.80'
PC STA = 1021+01.01
PT STA = 1023+41.20
SE = NC

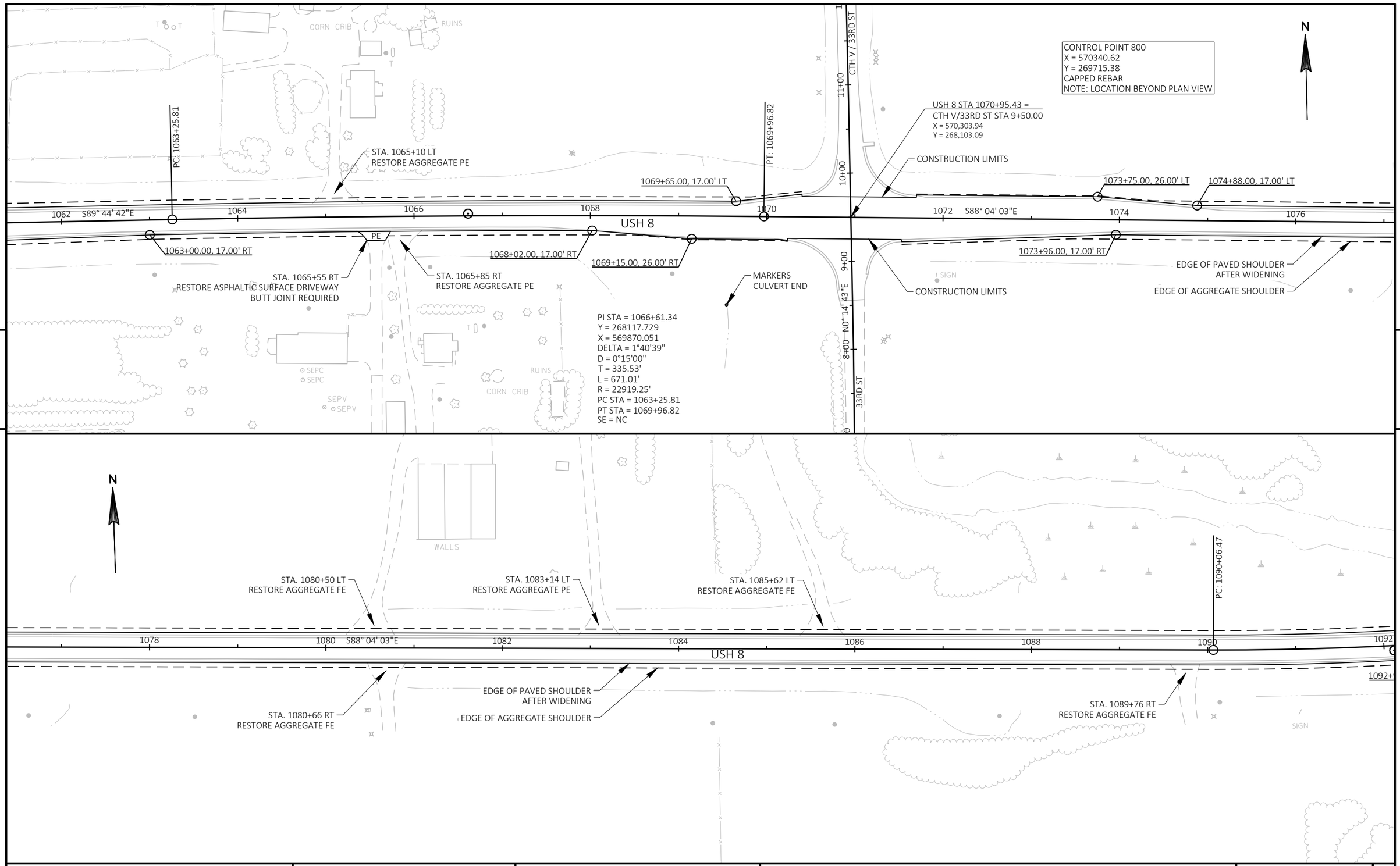
PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 59 E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-10

PLOT DATE : 10/8/2019 4:54 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########



PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 60	E

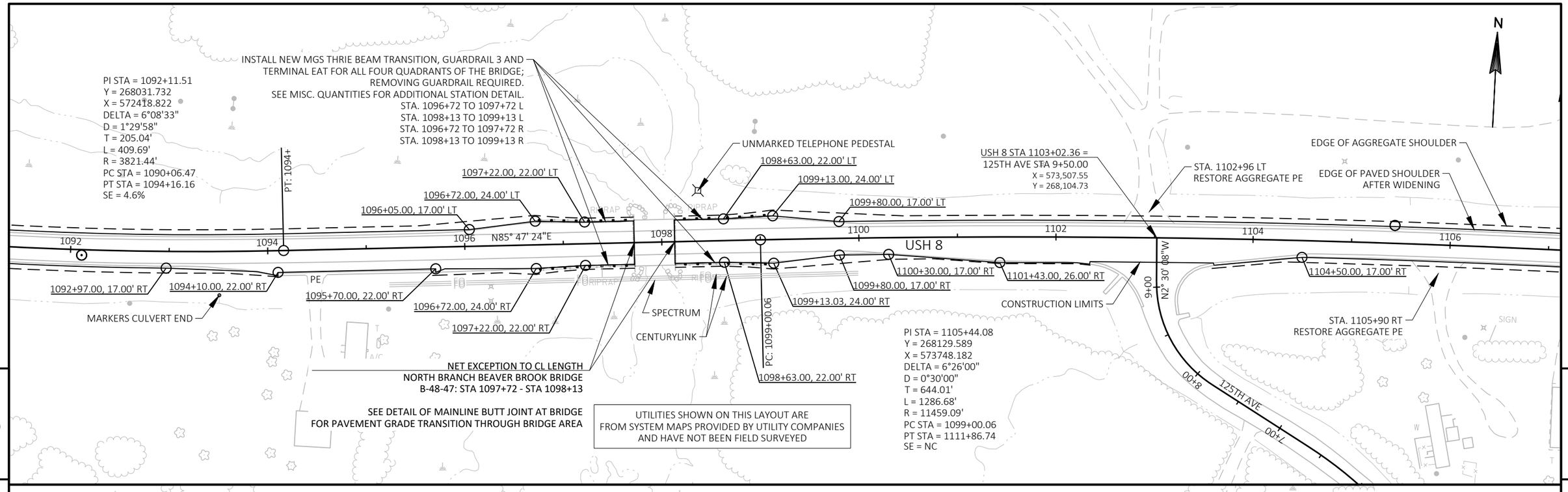


PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 61 E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-12

PLOT DATE : 10/8/2019 4:56 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

WISDOT/CADDS SHEET 44



PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 62	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-13

PLOT DATE : 1/3/2020 11:48 AM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

WISDOT/CADDS SHEET 44

PI STA = 1135+31.41
Y = 268092.797
X = 576736.149
DELTA = 0°32'13"
D = 0°15'00"
T = 107.39'
L = 214.77'
R = 22918.71'
PC STA = 1134+24.02
PT STA = 1136+38.80
SE = NC

PT: 1121+64.53

PC: 1134+24.02

PT: 1136+38.80

FE

1122 1124 1126 N89° 46' 44"E 1128 USH 8 1130 1132 1134 1136

N89° 46' 44"E

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

STA. 1130+95 LT
RESTORE AGGREGATE PE

N

5

5

PT: 1136+38.80

STA. 1136+60 LT
RESTORE AGGREGATE FE

STA. 1150+85 LT
RESTORE AGGREGATE FE

1138 1140 1142 1144 USH 8 1146 N89° 14' 31"E 1148 1150 1152

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

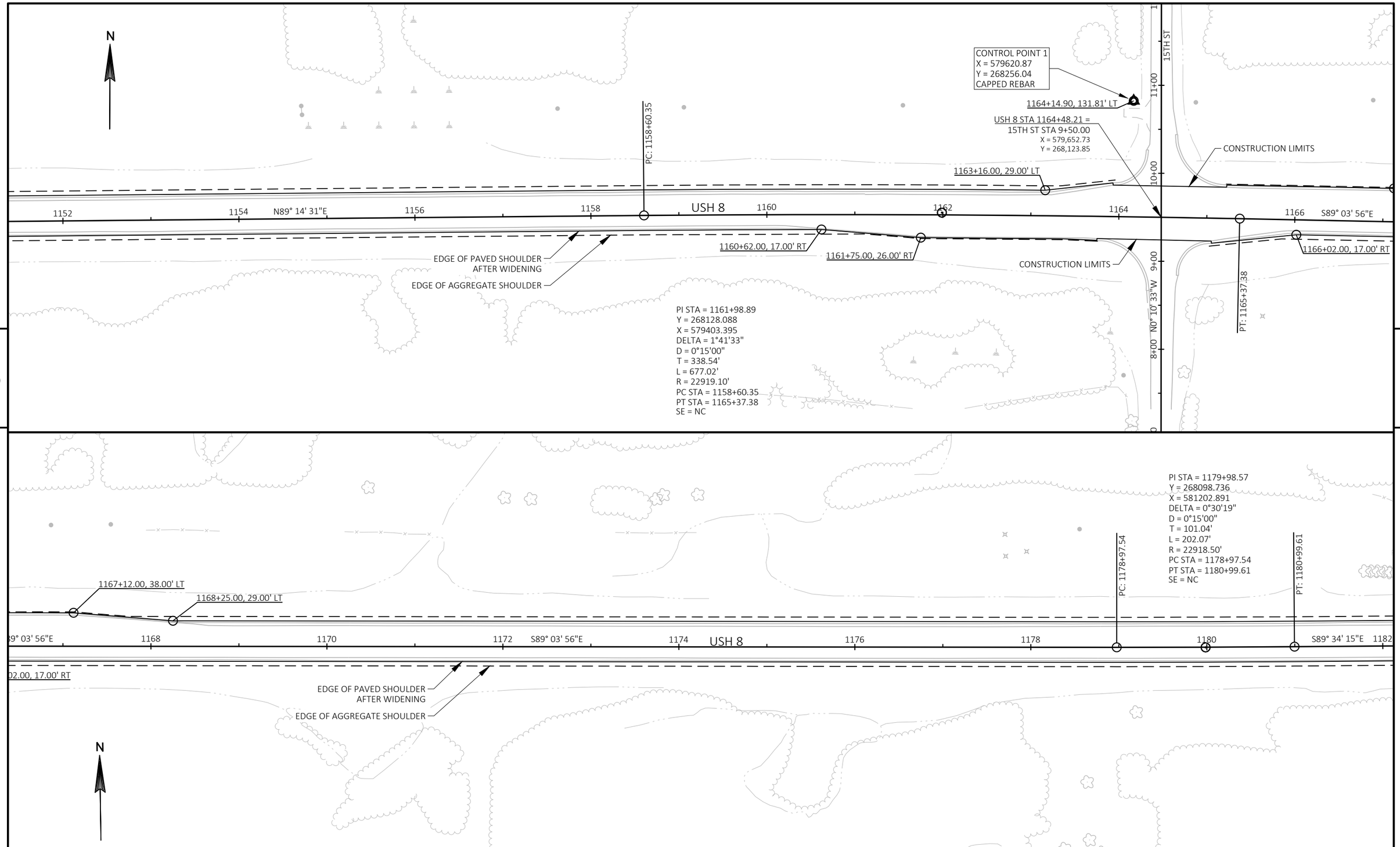
STA. 1150+75 RT
RESTORE AGGREGATE FE

N

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 63	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-14

PLOT DATE : 10/8/2019 4:57 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########



5

5

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 64	E

PI STA = 1190+62.08
Y = 268090.768
X = 582266.363
DELTA = 2°23'38"
D = 0°15'00"
T = 478.87'
L = 957.61'
R = 22919.96'
PC STA = 1185+83.20
PT STA = 1195+40.81
SE = NC

FLAG

STA. 1182+65 LT
RESTORE ASPHALTIC SURFACE DRIVEWAY
BUTT JOINT REQUIRED

CE

1185+00.00, 29.00' LT

PC: 1185+83.20

PT: 1195+40.81

1192+00.00, 17.00' LT

34' 15"E 1182 1184 S89° 34' 15"E 1186 USH 8 1188 1190 1192 1194 1196

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

N

5

5

RIPRAP

PT: 1204+98.06

SEE DETAIL OF PAVEMENT GRADE TRANSITION - RURAL TO URBAN
STA. 1210+67

STA. 1201+80 LT
RESTORE AGGREGATE FE

STA. 1208+90 LT
RESTORE AGGREGATE CE

1210+67.00, 34.00' LT

1206+28.00, 17.00' LT

1208+50.00, 23.00' LT

1198 1200 1202 1204 1206 USH 8 1208 S89° 33' 28"E 1210 1212

SIGN SIGN SIGN SIGN

STA. 1198+88 LT
RESTORE AGGREGATE FE

EDGE OF PAVED SHOULDER
AFTER WIDENING

EDGE OF AGGREGATE SHOULDER

STA. 1206+35 RT
RESTORE AGGREGATE FE

STA. 1206+28: END 'ASPHALTIC CENTERLINE
RUMBLE STRIPS 2-LANE RURAL'

STA. 1211+30 RT: END 'ASPHALTIC
SHOULDER RUMBLE STRIPS 2-LANE RURAL'

PI STA = 1200+19.51
Y = 268123.594
X = 583223.372
DELTA = 2°24'24"
D = 0°15'05"
T = 478.70'
L = 957.25'
R = 22788.55'
PC STA = 1195+40.81
PT STA = 1204+98.06
SE = NC

N

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 65	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-16

PLOT DATE : 10/8/2019 4:58 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

CONTROL POINT 401
X = 584858.54
Y = 268262.89
QO2341

1216+53.41, 151.91' LT

REPLACE DETERIORATED CURB & GUTTER

MARKERS CULVERT END

STA. 1215+80 LT: END 'ASPHALTIC SHOULDER RUMBLE STRIP 2-LANE RURAL'

1214+70.00, 51.00' LT

1215+95.00, 51.00' LT

CONSTRUCTION LIMITS

REPLACE DETERIORATED CURB & GUTTER

USH 8 STA 1217+38.94 =
CTH T STA 9+50.00
X = 584,942.89
Y = 268,110.32

UTILITIES SHOWN ON THIS LAYOUT ARE FROM FIELD SURVEY AFTER LOCATE

OVERHEAD ELECTRIC

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

1218+65.00, 54.00' LT

CENTURYLINK

WE ENGERGIES

USH 8/63

S89° 33' 28"E

1213+08.00, 26.00' RT

1211+95.00, 17.00' RT

MARKERS CULVERT END

USH 8 STA 1217+33.11 =
USH 63 S STA 9+50.00
X = 584,937.07
Y = 268,110.36

CONST. LIMITS

OVERHEAD ELECTRIC

1220+00.00, 22.00' RT

1220+06.00, 44.50' RT

1218+51.00, 22.00' RT

MARKERS CULVERT END

NORTHWEST COMMUNICATIONS

1222+00.00, 34.00' RT

1223+40.00, 34.00' RT

N 0° 18' 27"E

USH 63 S

MARKERS CULVERT END

STA. 1240+55 LT
RESTORE AGGREGATE PE

EDGE OF AGGREGATE SHOULDER

EDGE OF PAVED SHOULDER

MARKERS CULVERT END

1240+60.00, 54.00' LT

S89° 25' 11"E

USH 8/63

USH 8 STA 1230+76.39 =
WESTERN BLVD STA 9+50.00
X = 586,280.30
Y = 268,099.99

CONSTRUCTION LIMITS

1239+65.00, 34.00' RT

MARKERS CULVERT END

CONSTRUCTION LIMITS

PROSSER AVE

N 0° 26' 32"E

WESTERN BLVD

ANTIQUE RD

FILLER CAPS

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 66	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-17

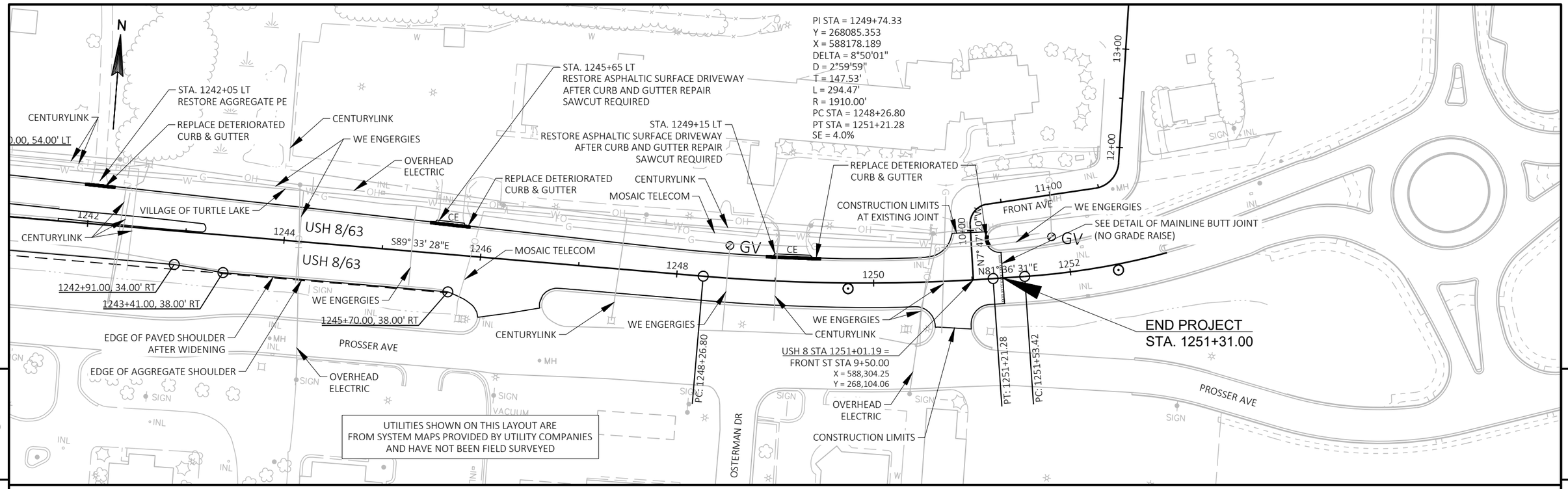
PLOT DATE : 10/8/2019 4:59 PM

PLOT BY : ERICKSON, ZACHARY A

PLOT NAME :

PLOT SCALE : ##########

WISDOT/CADDS SHEET 44



5

5

PROJECT NO: 1570-00-74 & 1570-00-77	HWY: USH 8	COUNTY: POLK	PLAN	SHEET 67	E

FILE NAME : N:\PDS\C3D\15700004\SHEETSPLAN\15700004_PP.DWG
LAYOUT NAME - PP-18

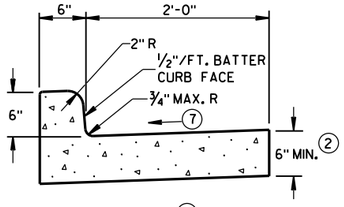
PLOT DATE : 10/8/2019 4:59 PM PLOT BY : ERICKSON, ZACHARY A PLOT NAME : PLOT SCALE : ##########

WISDOT/CADDS SHEET 44

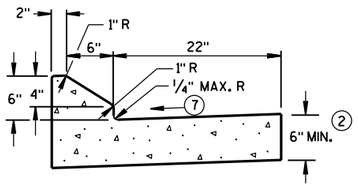
Standard Detail Drawing List

08D01-20A	CONCRETE CURB & GUTTER
08D01-20B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08E15-01	CULVERT PIPE CHECK
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13A10-02A	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-02C	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-02D	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A11-03A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-03B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13C19-01	HMA LONGITUDINAL JOINTS
14B42-06A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-06B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-06C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-06D	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-04A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B45-05A	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05B	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05C	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05D	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05E	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05F	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05G	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05H	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05I	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05J	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05K	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-05L	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C06-09	SIGNING & MARKING FOR TWO LANE BRIDGES
15C07-14B	PAVEMENT MARKING WORDS
15C07-14C	PAVEMENT MARKING ARROWS
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15C08-19C	PAVEMENT MARKING (TURN LANES)
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C18-04	MEDIAN ISLAND MARKING
15C19-05A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C19-05B	MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY
15C27-03B	PAVEMENT MARKING (ISLANDS)
15C33-03	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-03A	PAVEMENT MARKING (INTERSECTIONS)
15C35-03B	PAVEMENT MARKING AND SIGNING (CLIMBING LANE & PASSING LANE)
15C35-03C	PAVEMENT MARKING AND SIGNING (CLIMBING LANE & PASSING LANE)
15D20-04	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D21-06	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-01	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

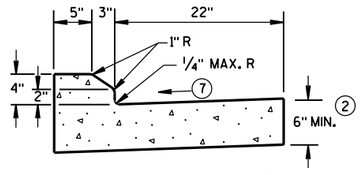
6



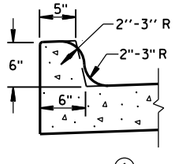
TYPES A (1) & D



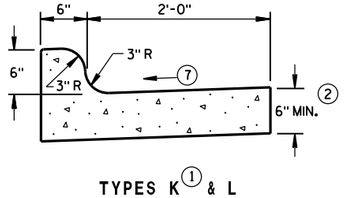
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

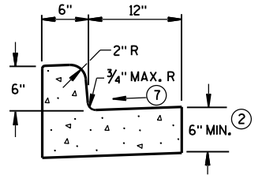


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



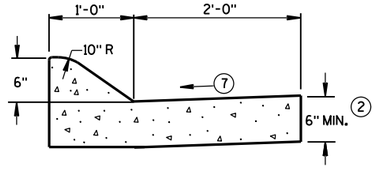
TYPES K (1) & L

CONCRETE CURB & GUTTER 30"

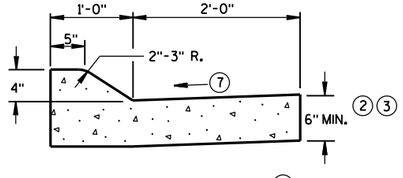


TYPES A (1) & D

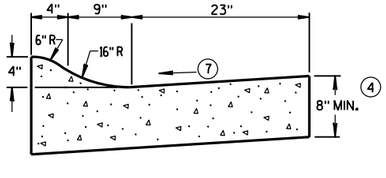
CONCRETE CURB & GUTTER 18"



6" SLOPED CURB TYPES A (1) & D

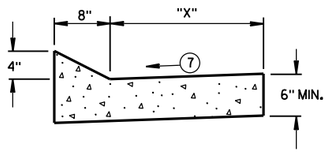


4" SLOPED CURB TYPES A (1) & D



4" SLOPED CURB TYPES R (1) & T (5)

CONCRETE CURB & GUTTER 36"



TYPES TBT & TBTT (1)

CONCRETE CURB & GUTTER

TBT & TBTT	"X"
30"	22"
36"	28"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

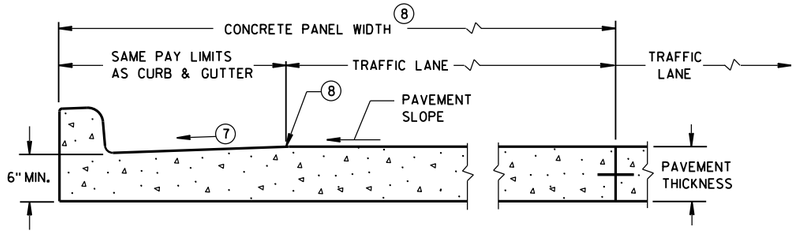
(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

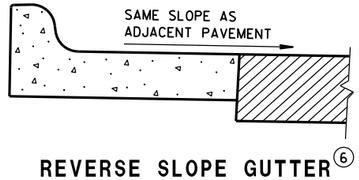
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB & GUTTER

* BIKE LANE IS NOT SHOWN.



REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

CONCRETE CURB & GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPO...

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S.D.D. 8 D 1-20a

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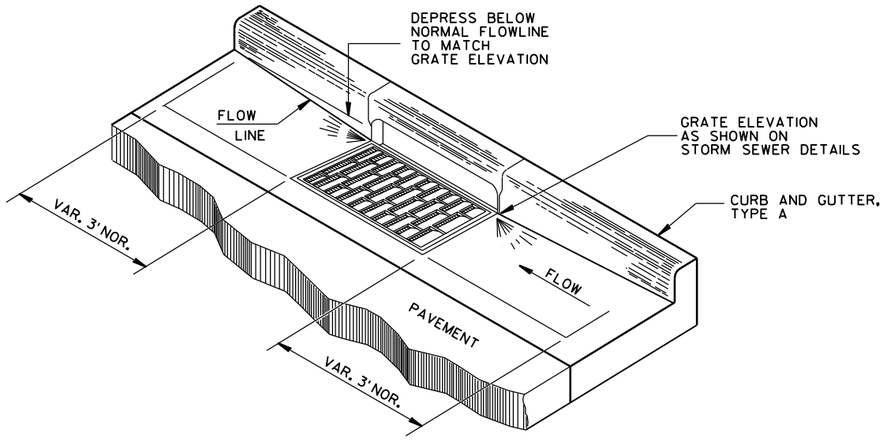
S.D.D. 8 D 1-20a



DETAIL OF CURB AND GUTTER AT INLETS

(TYPE H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

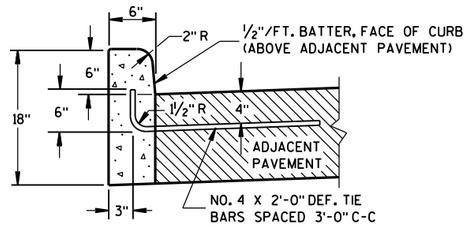
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

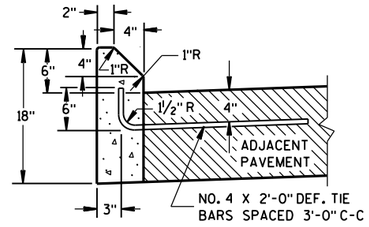
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(9) REFER TO SDD 8D18 AND SDD 8D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

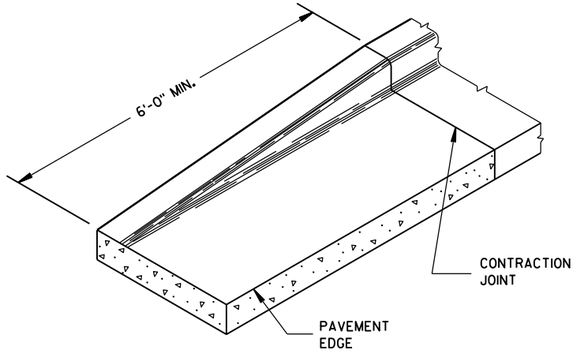


TYPES A(1) & D

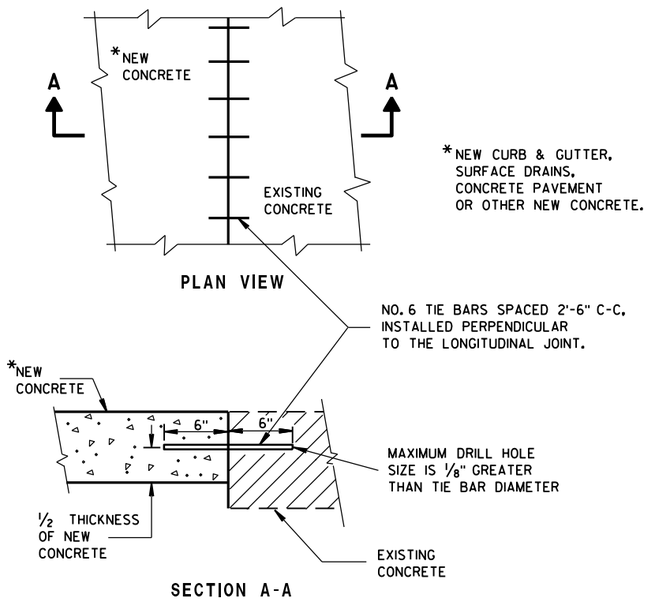


TYPES G(1) & J

CONCRETE CURB



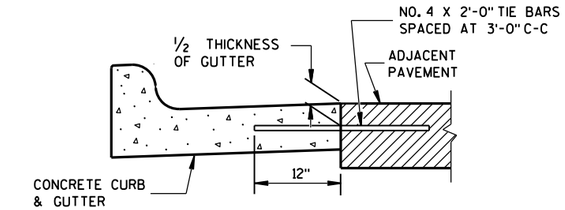
END SECTION CURB & GUTTER



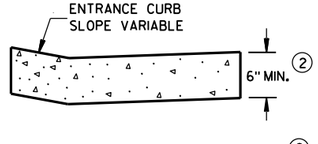
PLAN VIEW

SECTION A-A

TIE BARS DRILLED INTO EXISTING PAVEMENT



TYPICAL TIE BAR LOCATION (1)



DRIVEWAY ENTRANCE CURB (9)

(WHEN DIRECTED BY THE ENGINEER)

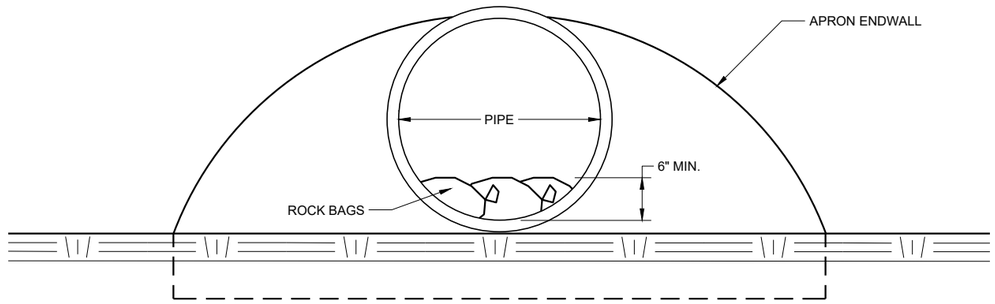
CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

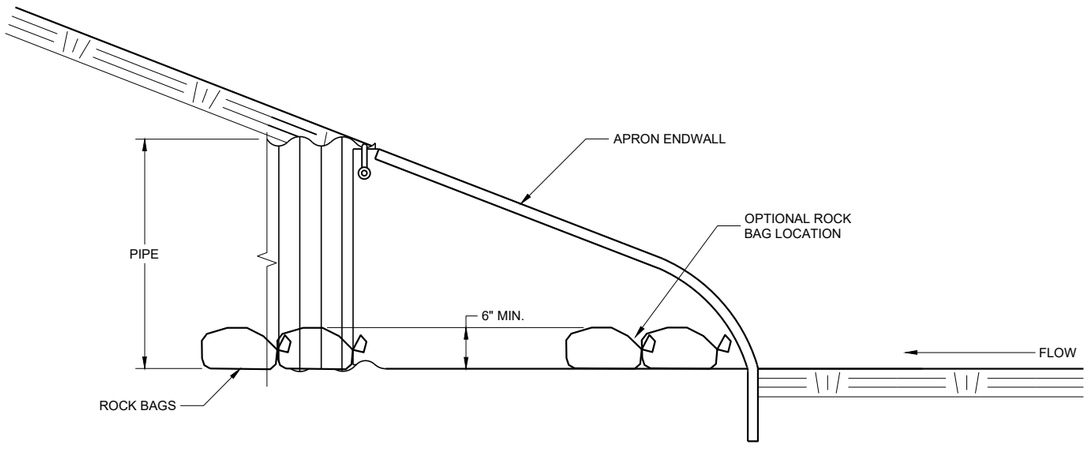
APPROVED
June, 2017 — DATE
/S/ Rodney Taylor
ROADWAY STANDARDS 70 ENT
UNIT SUPERVISOR
FHWA

6

S.D.D. 8 D 1-20b



END VIEW



SIDE VIEW

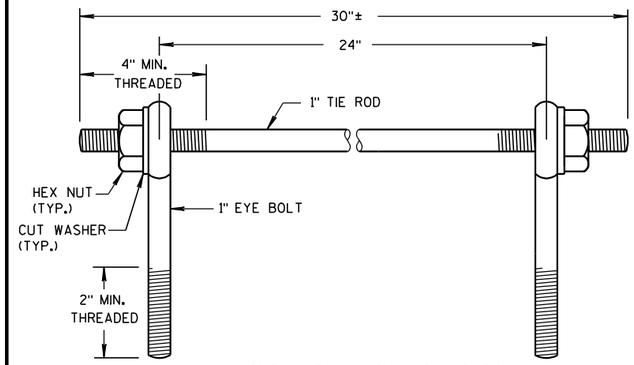
CULVERT PIPE CHECK

(INSTALL ON INLET END ONLY)

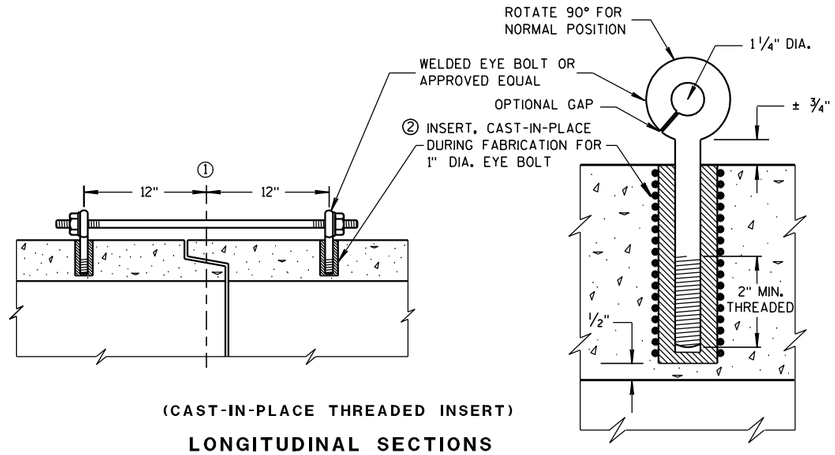
CULVERT PIPE CHECK	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Daniel Schave EROSION CONTROL ENGI
FHWA	

6

SDD 08E15 - 01



EYE BOLTS AND TIE ROD



(CAST-IN-PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

① ℄ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

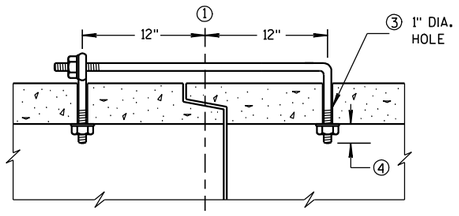
② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ℄ OF TONGUE AND GROOVE.

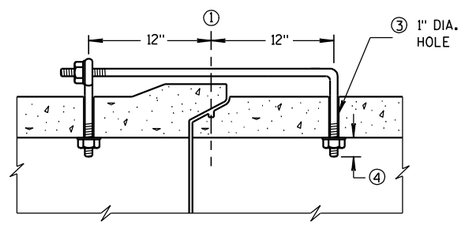
④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.

⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.



(TONGUE & GROOVE PIPE)

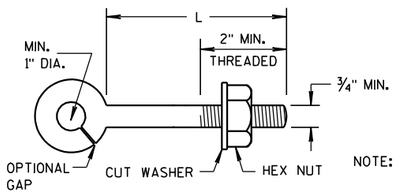


(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

EYE BOLT DIMENSION TABLE

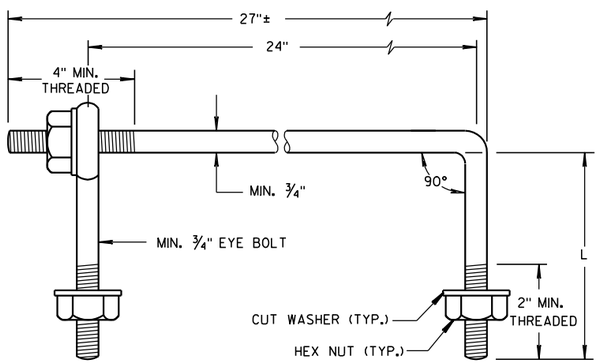
PIPE SIZE	L = LENGTH	
	TONGUE & GROOVE PIPE	MODIFIED BELL PIPE
18" TO 24"	4 1/2"	6 1/4"
30"	5"	7"
36"	5 1/2"	7"
42"	6"	
48"	6 1/2"	
60"	7 1/2"	
66"	8"	



NOTE:

EYE BOLT

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD

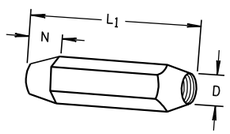
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12-60	5/8	5/8	5	1/2
66-84	3/4	3/4	5	1/2
90-108	1	1	7	1 1/16

DIMENSIONS SHOWN ARE IN INCHES



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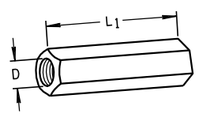
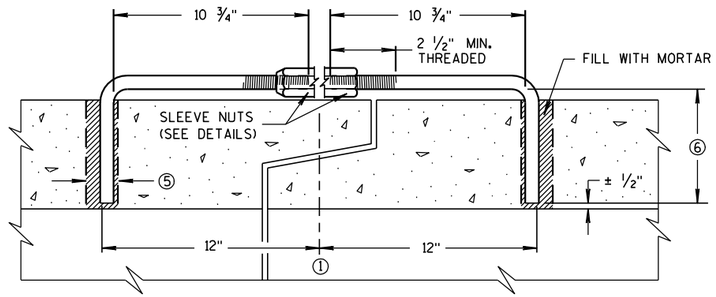


PLAIN

RIGHT AND LEFT THREADS

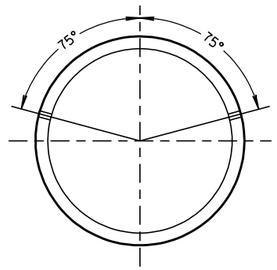
SLEEVE NUTS



LONGITUDINAL SECTION

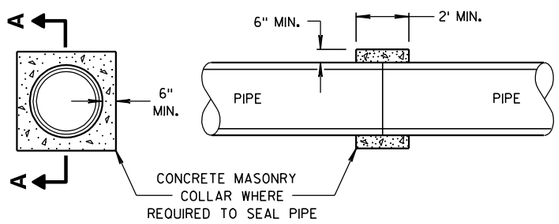
(JOINT TIES FOR 12" TO 108" DIA. CONCRETE PIPE)

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A-A

CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/5/2012 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
72
FHWA

6

S.D.D. 8 F 4-7

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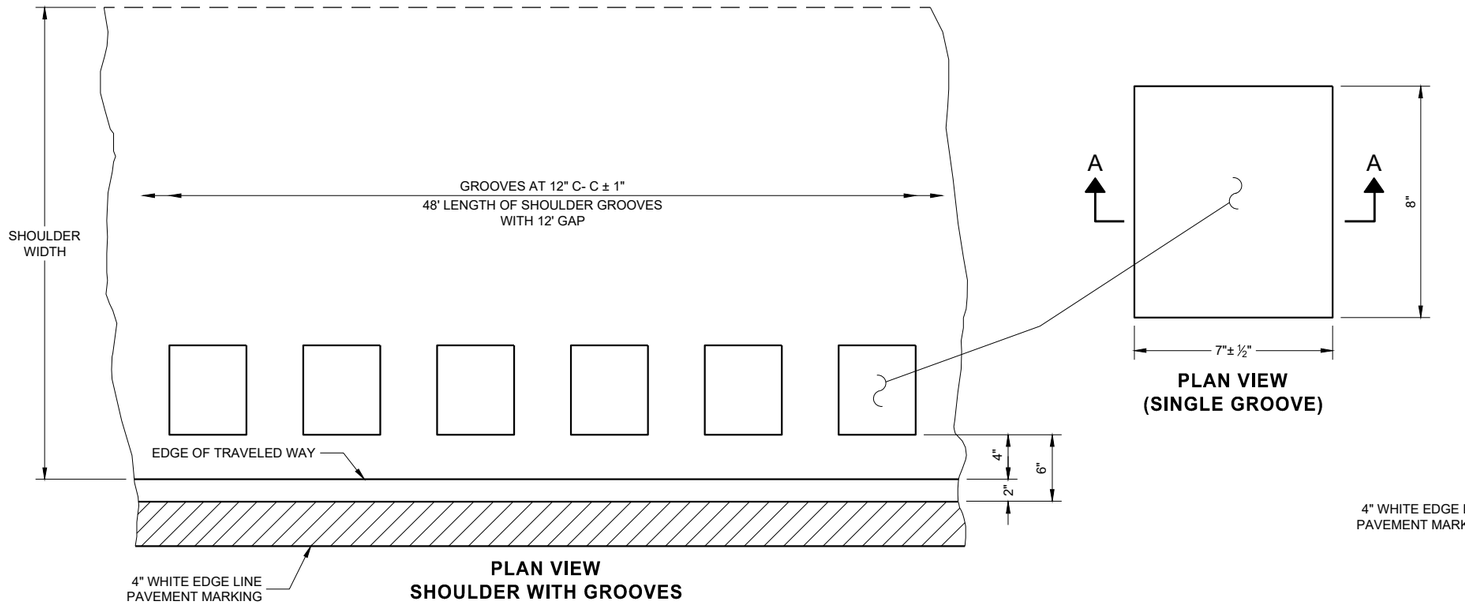
S.D.D. 8 F 4-7



PLAN VIEW
SHOULDER WITH GROOVES

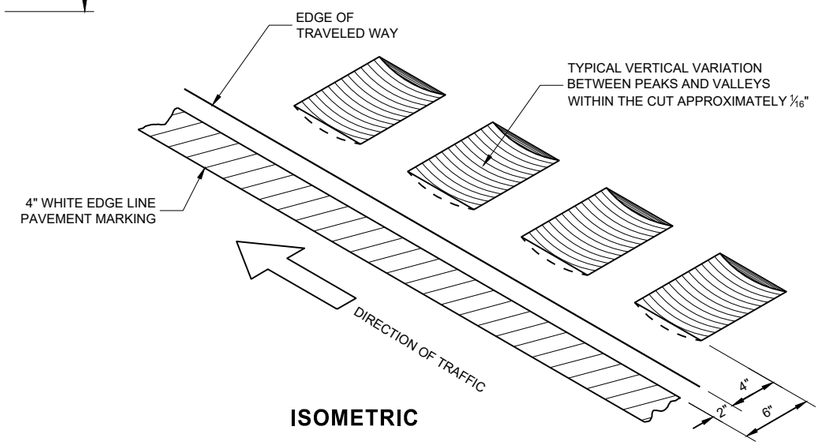
PLAN VIEW
(SINGLE GROOVE)

GENERAL NOTES

DETAILS OF CONSTRUCTION NO SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

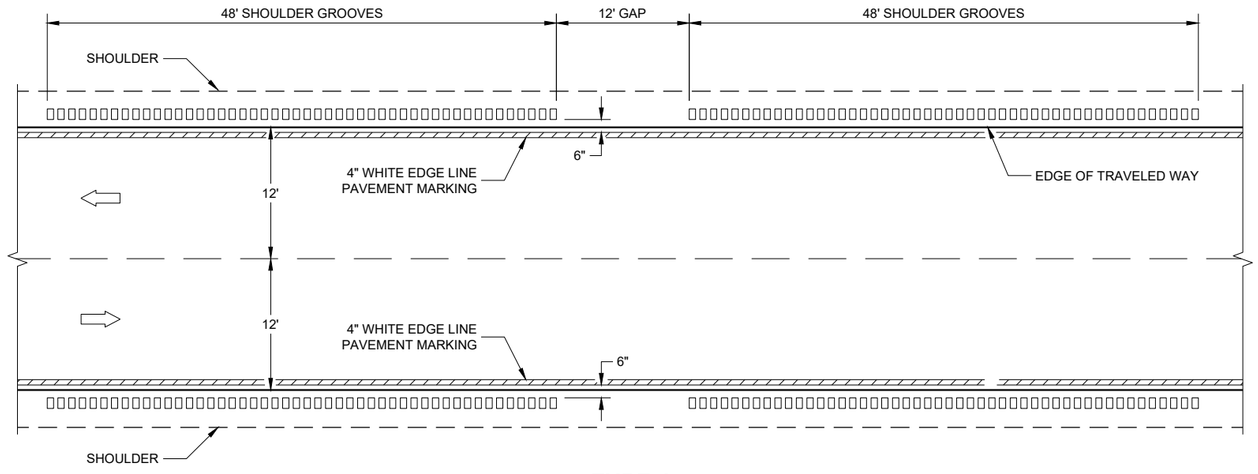
DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

(1) SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.

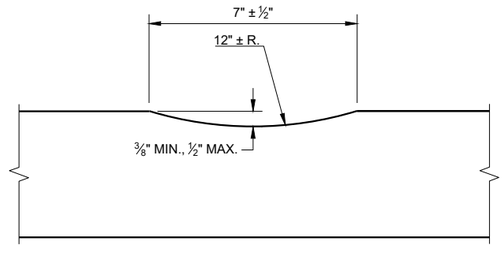


ISOMETRIC

PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP



TYPE 1
2 - LANE SHOULDER RUMBLE STRIP



SECTION A - A

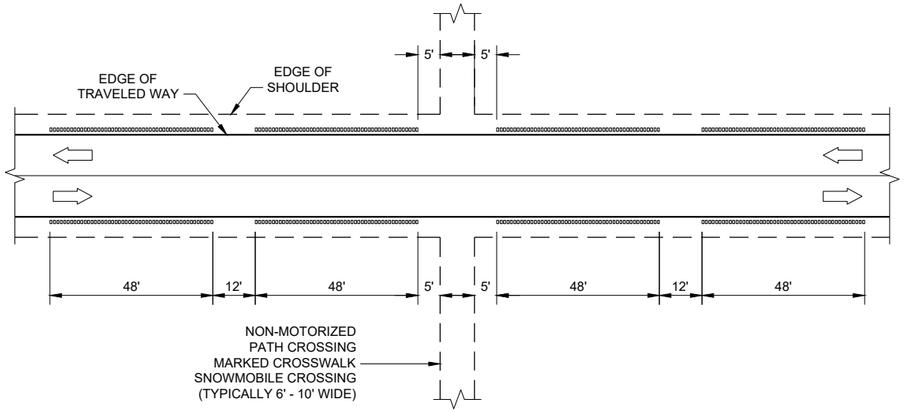
2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

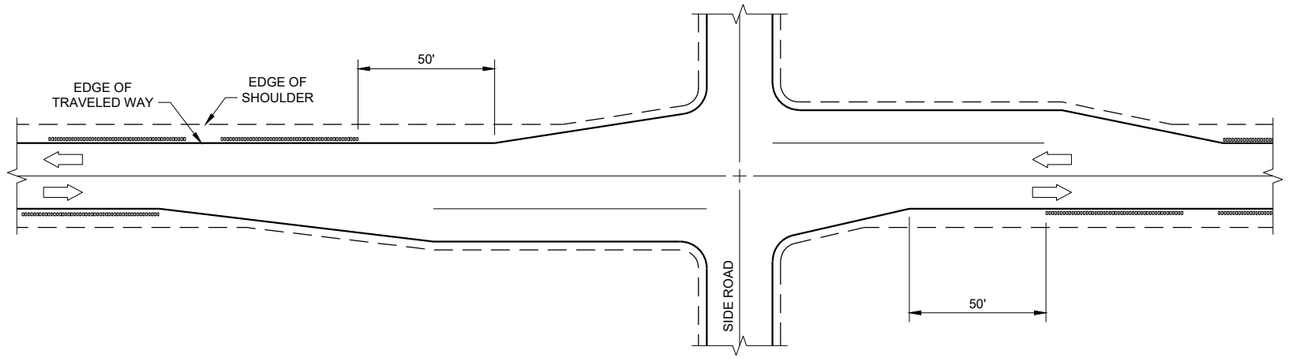
SDD 13A10 - 02a

6

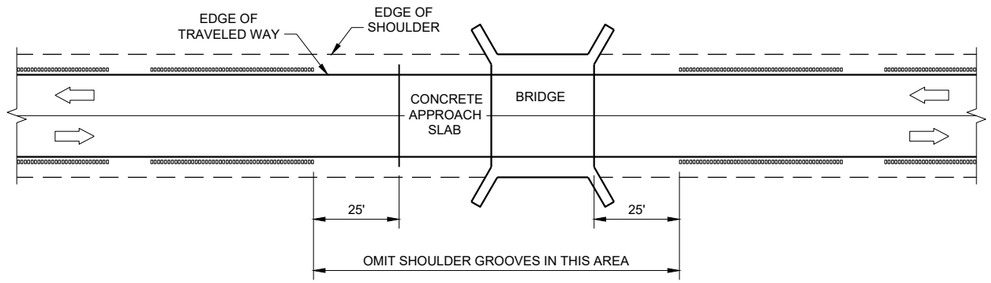
SDD 13A10 - 02a



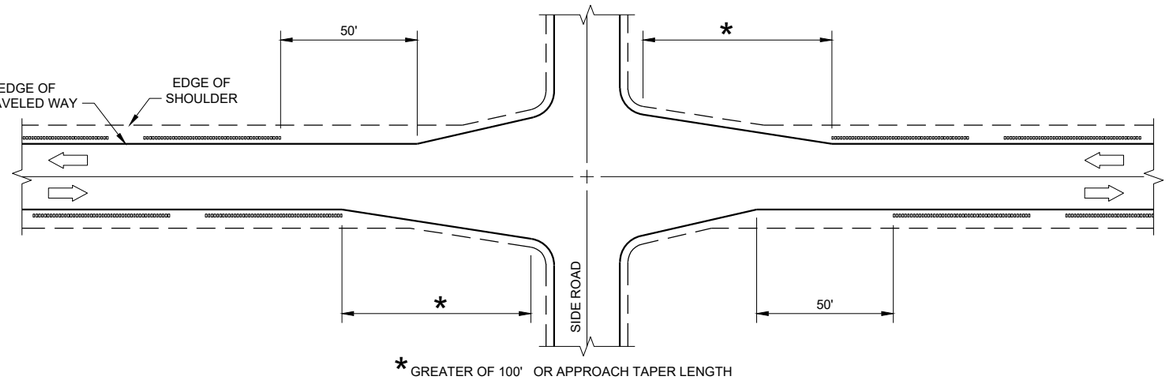
SHOULDER GROOVES AT MISCELLANEOUS CROSSINGS



SHOULDER GROOVES AT RIGHT TURN LANE

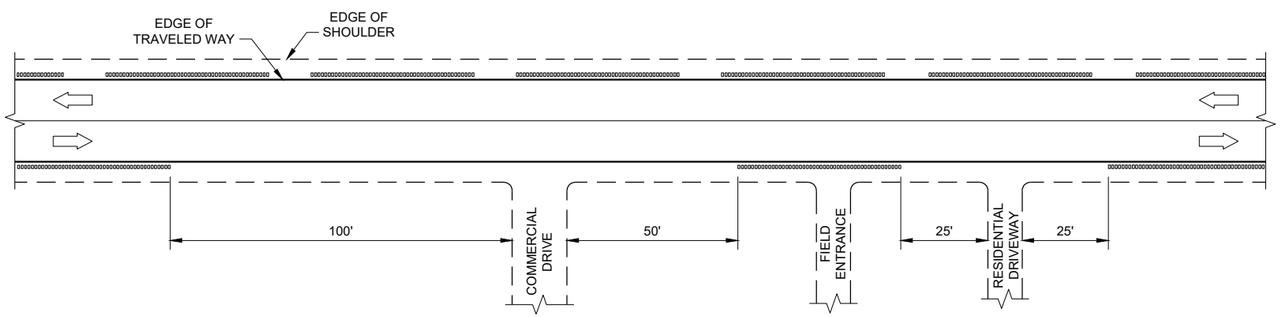


SHOULDER GROOVES AT BRIDGES



* GREATER OF 100' OR APPROACH TAPER LENGTH

SHOULDER GROOVES AT INTERSECTIONS WITH APPROACH TAPER



SHOULDER GROOVES AT DRIVEWAYS (1)

GENERAL NOTES

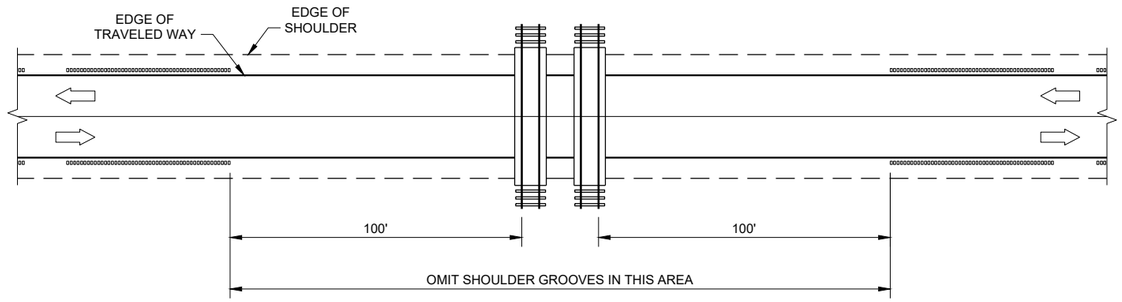
(1) SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.

2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING

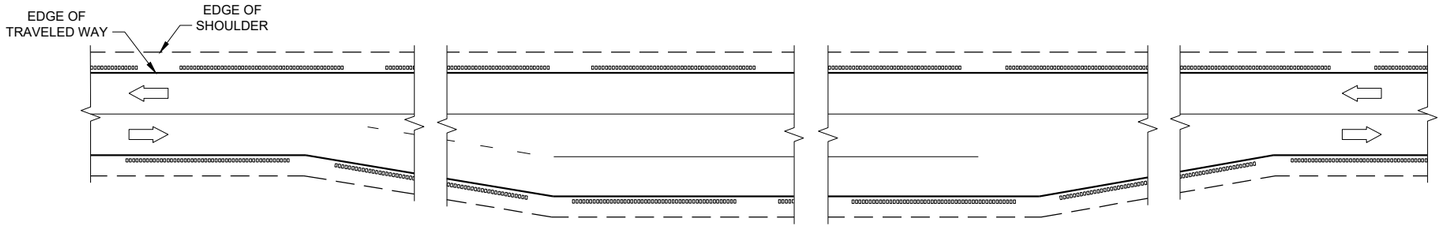
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

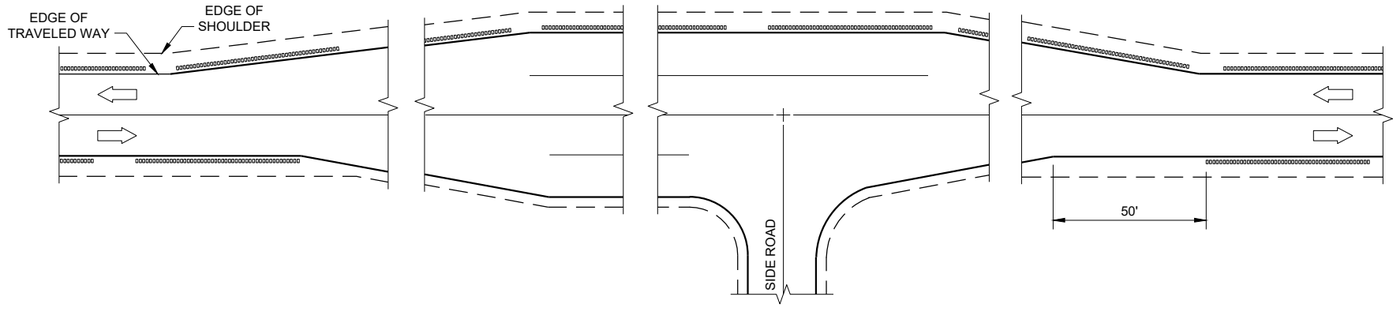
SDD 13A10 - 02c



SHOULDER GROOVES AT RAILROADS



SHOULDER GROOVES AT PASSING AND CLIMBING LANES



SHOULDER GROOVES AT BYPASS LANES

6

6

SDD 13A10 - 02d

SDD13A10 - 02d

2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING

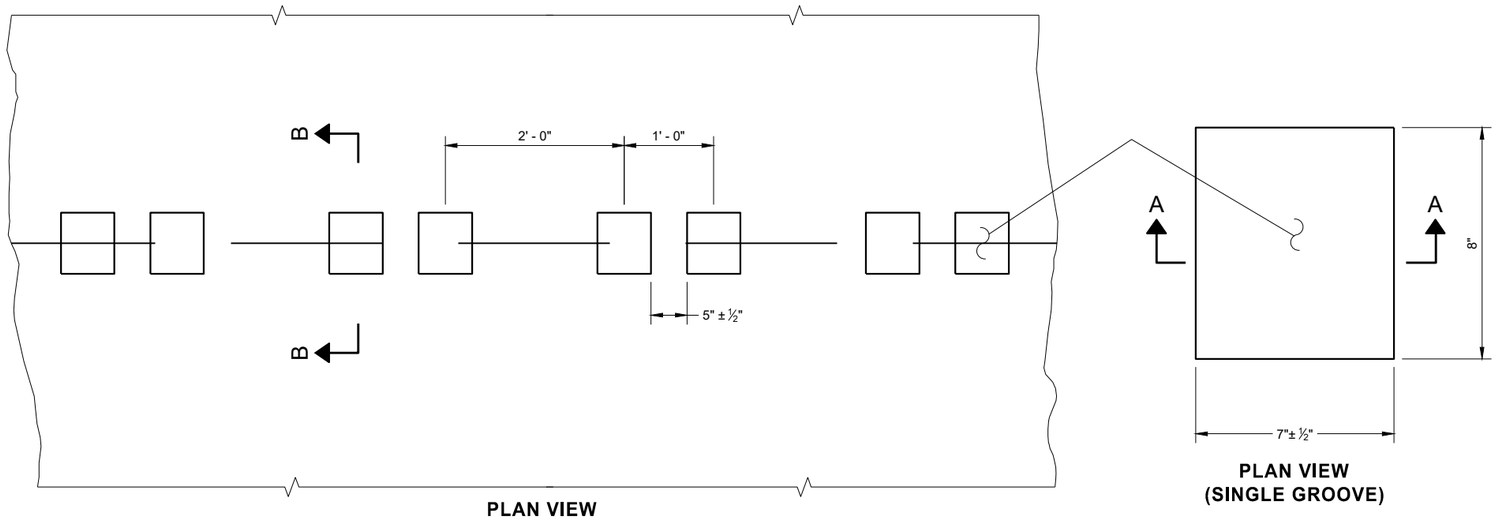
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

7/2018
DATE

/S/ Rodney Taylor
ROADWAY STANDARDS DEVE ENGINEER

FHWA



PLAN VIEW
SHOULDER WITH GROOVES

PLAN VIEW
(SINGLE GROOVE)

PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP

GENERAL NOTES

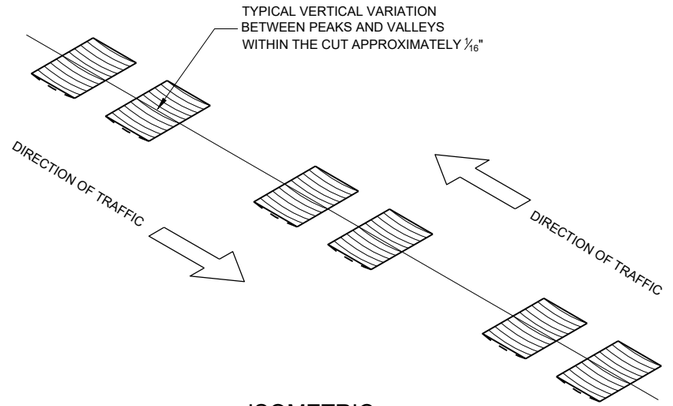
DETAILS OF CONSTRUCTION NO SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

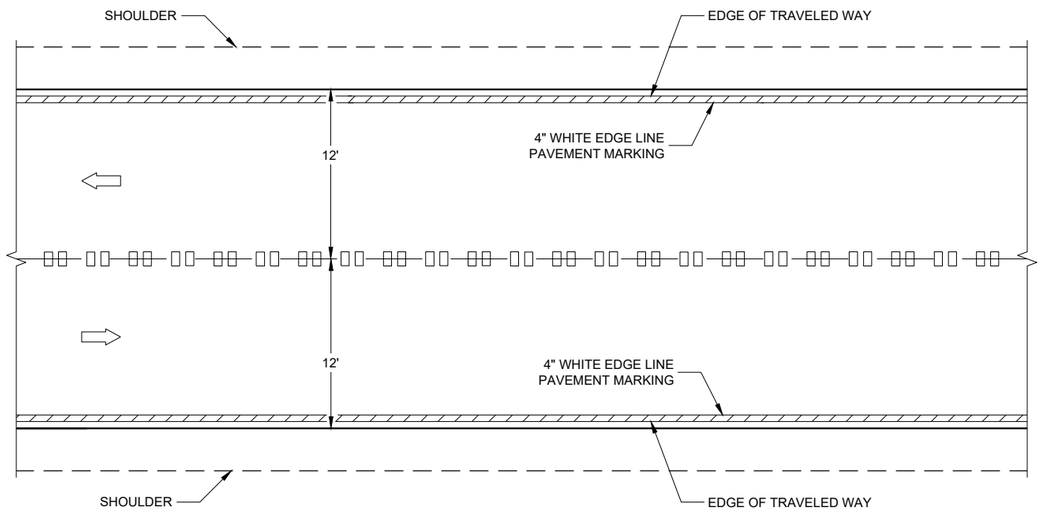
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

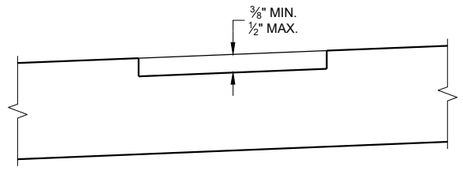
(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



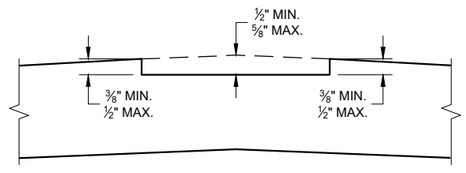
ISOMETRIC



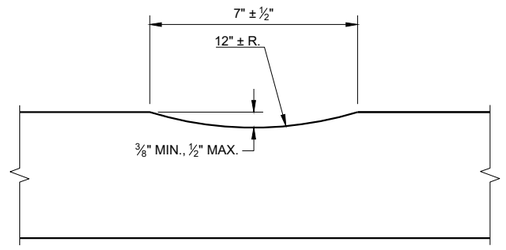
CENTERLINE GROOVES ON TWO-WAY ROADWAYS



SECTION B - B
SUPERELEVATED ROADWAY



SECTION B - B
CROWNED ROADWAY



SECTION A - A

2-LANE RURAL
CENTER LINE RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 13A11 - 03a

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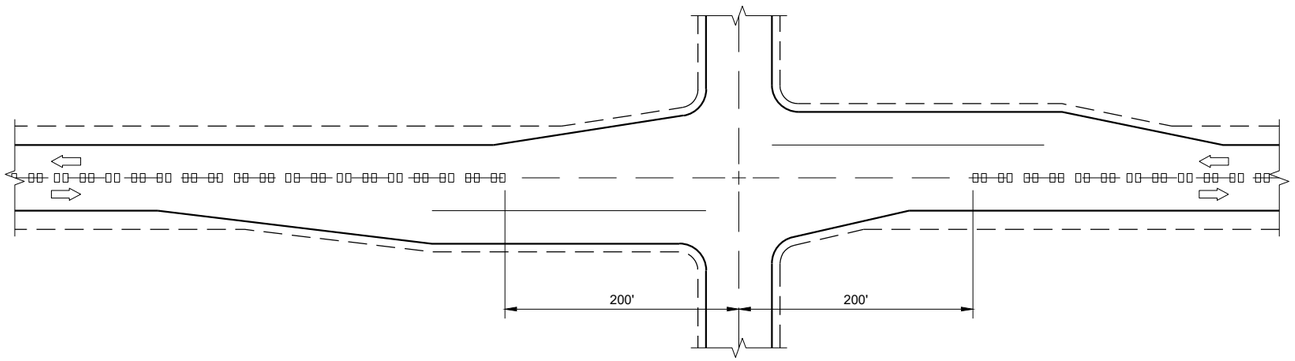
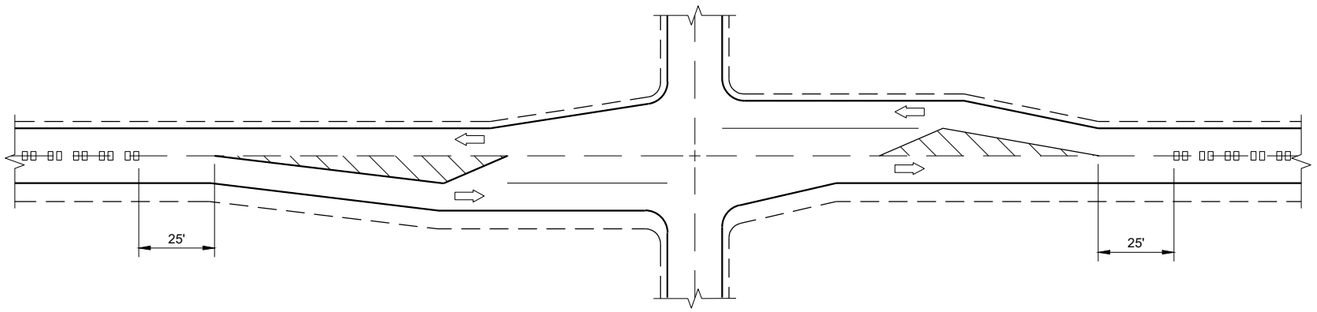
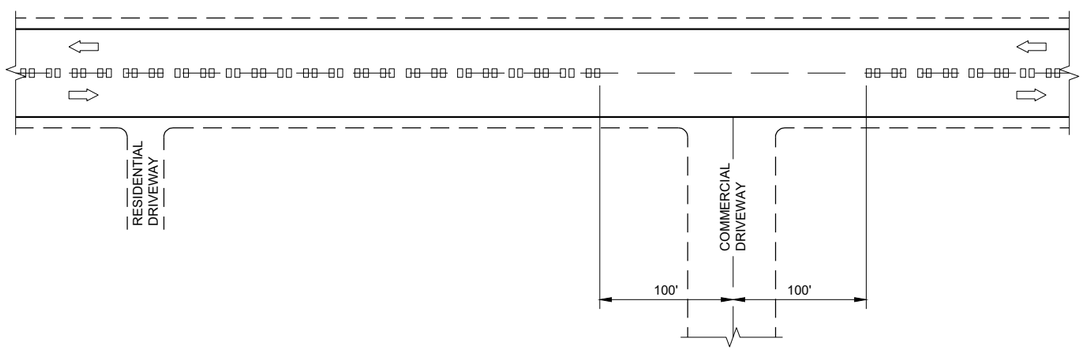
SDD 13A11 - 03a



CENTERLINE GROOVES AT INTERSECTIONS



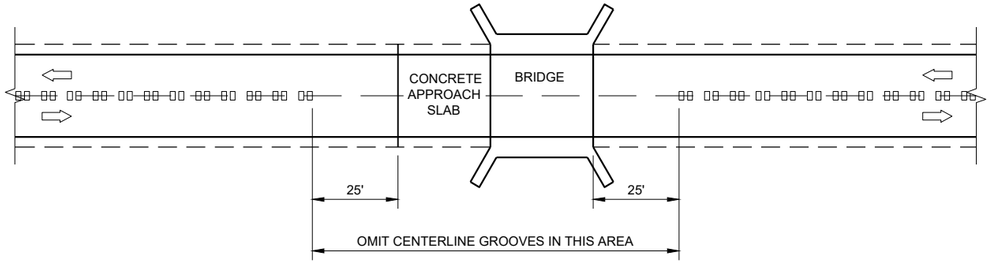
CENTERLINE GROOVES AT INTERSECTIONS (WITH LEFT TURN LANES)



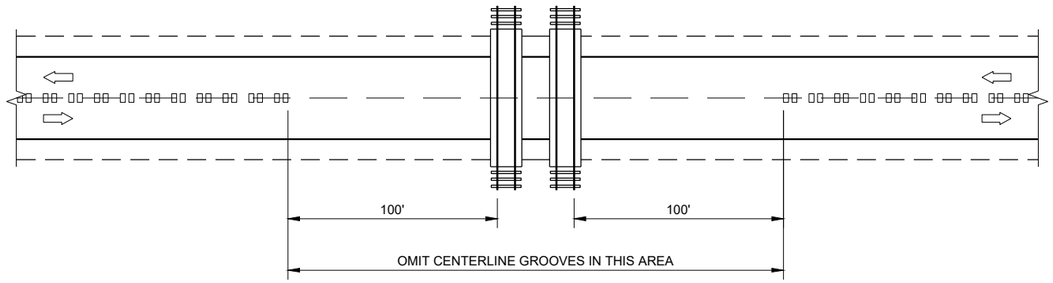
CENTERLINE GROOVES AT DRIVEWAYS (1)

GENERAL NOTES

(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

2-LANE RURAL CENTERLINE RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

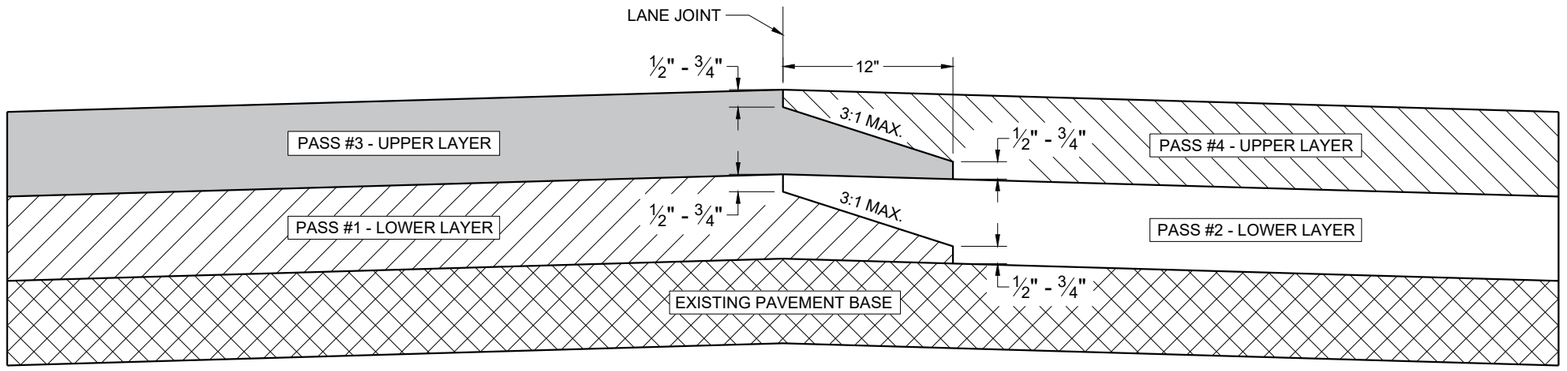
APPROVED
7/2018
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVE ENGINEER

FHWA

6
SDD 13A11 - 03b

GENERAL NOTES

CONFORM TO STANDARD SPECIFICATION 450.3.2.8



TYPICAL PAVEMENT CROSS SECTION
OF NOTCHED WEDGE LONGITUDINAL JOINTS

6

6

SDD 13C19 - 01

SDD 13C19 - 01

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE

/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA

(1) WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.

(2) USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.

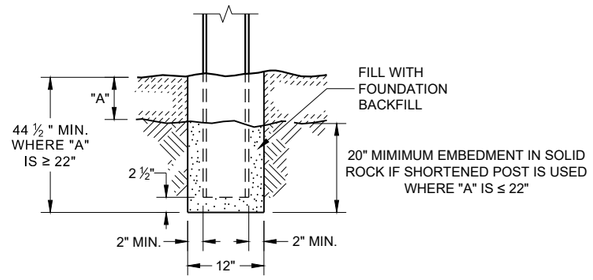
(3) IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.

(4) WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).

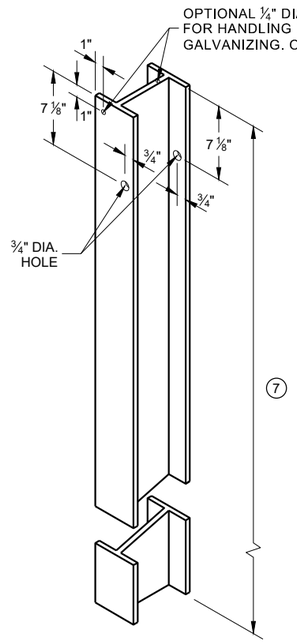
(5) FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS +1". FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 ¾" TO 32".

(6) WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

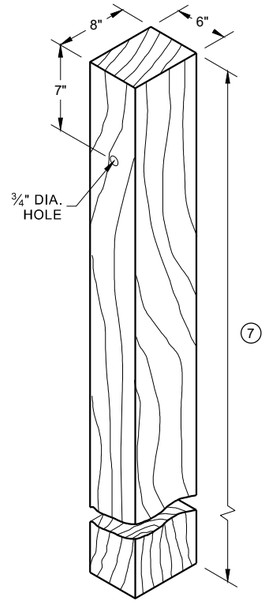
(7) TOTAL POST LENGTH FOR TYPE K IS 7' - 0".
TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".



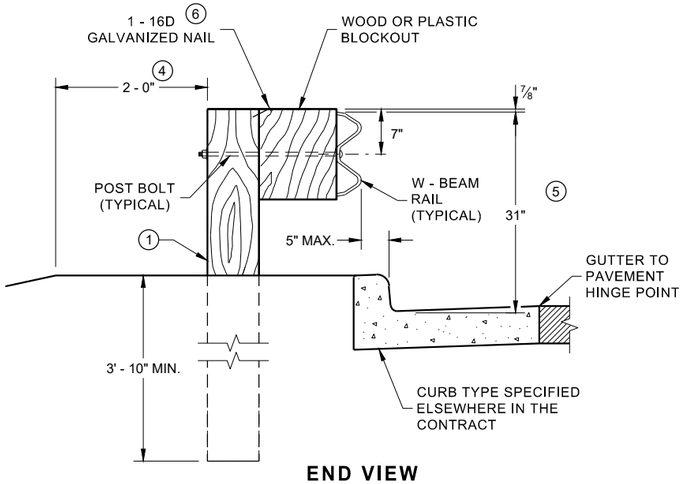
END VIEW
SETTING STEEL OR WOOD POST IN ROCK (3)



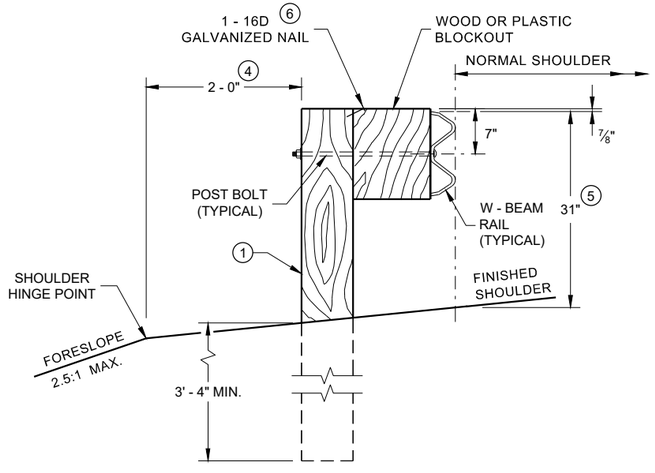
STEEL POST & HOLE PUNCHING DETAIL (W 6 X 9) (1)



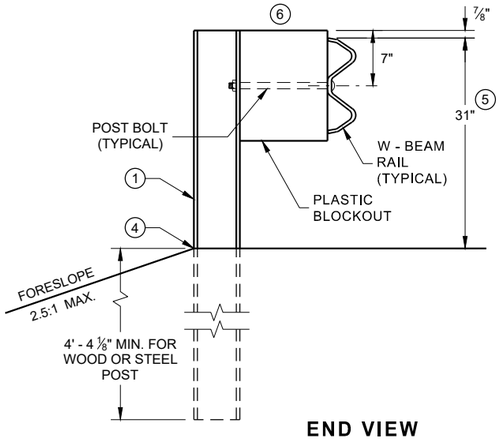
WOOD POST (6" X 8") NOMINAL (1)



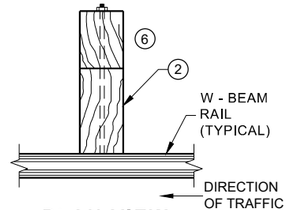
END VIEW
LOCATED ALONG A CURBED ROADWAY



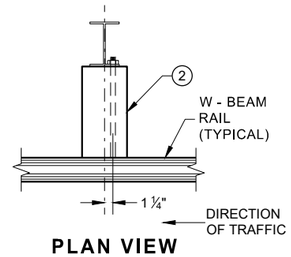
END VIEW
LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION



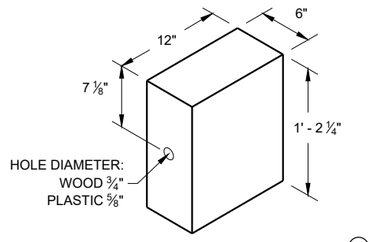
END VIEW
MGS LONGER POST AT HALFPOST SPACING W BEAM (K)



PLAN VIEW
WOOD POST, BLOCKOUT & BEAM



PLAN VIEW
STEEL POST, PLASTIC BLOCKOUT & BEAM



WOOD OR PLASTIC BLOCKOUT (2)

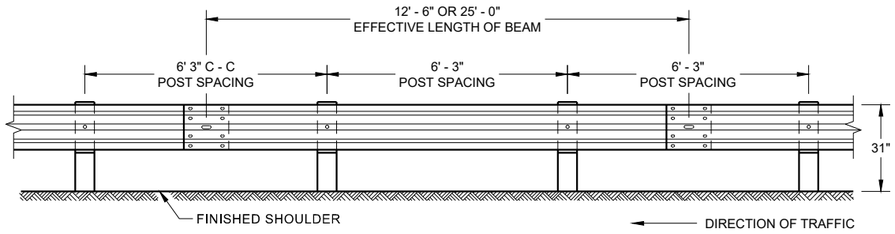
MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

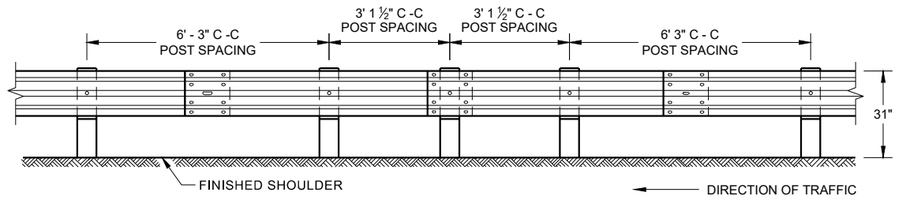
SDD 14B42 - 06a

6

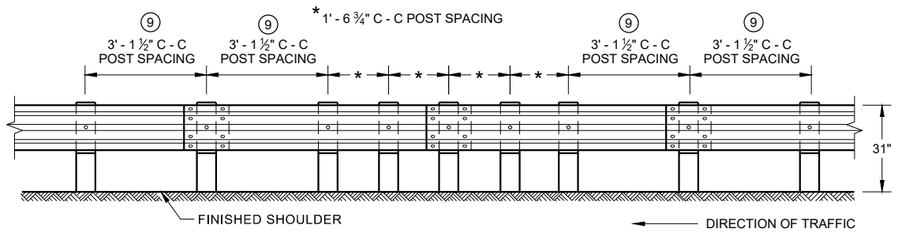
SDD 14B42 - 06a



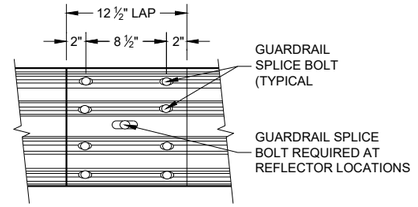
FRONT VIEW
POST SPACING STANDARD INSTALLATION



FRONT VIEW
HALF POST SPACING (HS) AND
HALF POST SPACING WITH LONGER POSTS (K)



FRONT VIEW
QUARTER POST SPACING (QS)



FRONT VIEW
MID-SPAN BEAM SPLICE

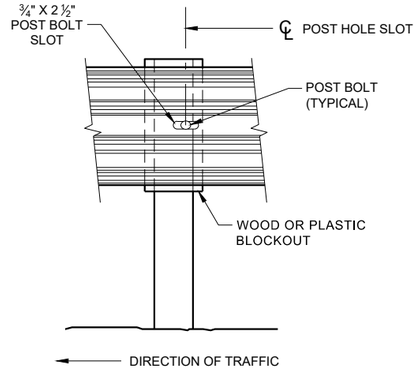
GENERAL NOTES

⑧ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL. RAIL SPLICE LOCATIONS ARE THE ONLY ACCEPTABLE LOCATIONS FOR REFLECTORS.

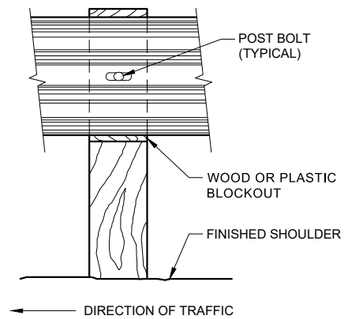
⑨ 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.

POST BOLTS ARE A ¾" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES ⅝" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND ⅝" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.

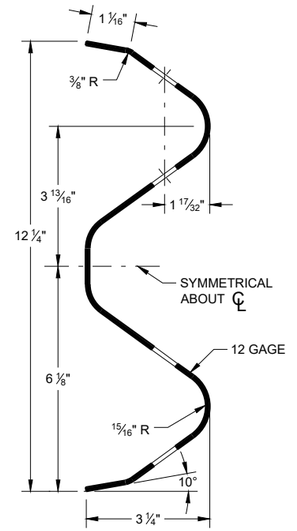
GUARD RAIL SPLICE BOLTS ARE A ⅝" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES ⅝" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.



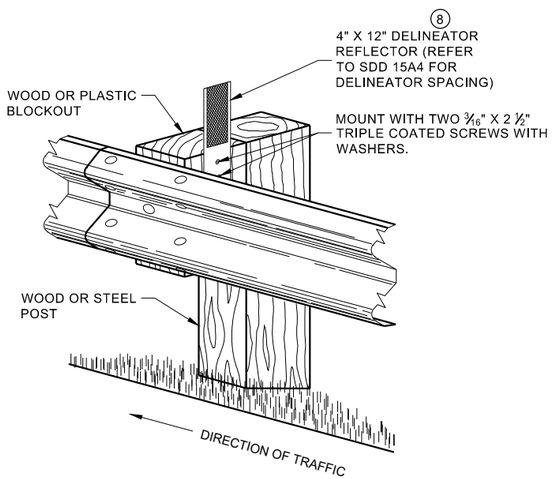
FRONT VIEW AT STEEL POST



FRONT VIEW AT WOOD POST



SECTION THRU W-BEAM RAIL



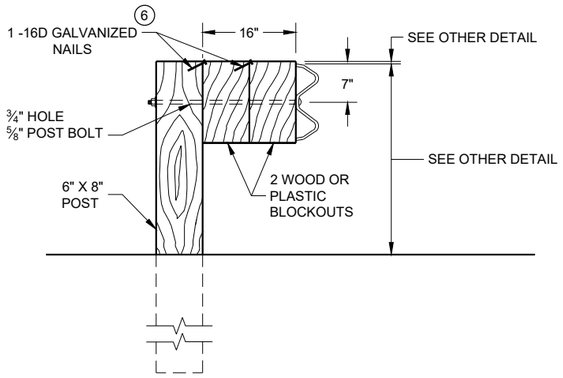
ONE SIDED REFLECTOR DETAIL
AND TYPICAL INSTALLATION

MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 14B42 - 06b

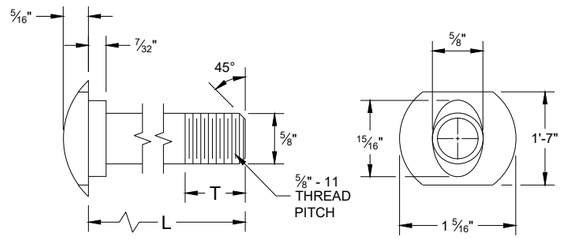


DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

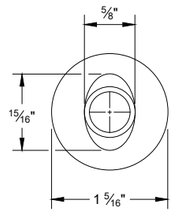
NOTE:

1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 3⁄16".
2. IF THE BOLT EXTENDS MORE THAN ¼" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

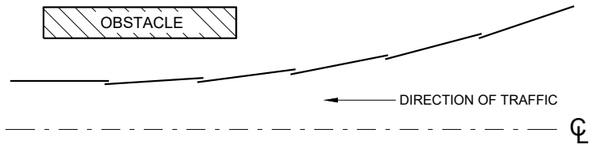


POST BOLT TABLE

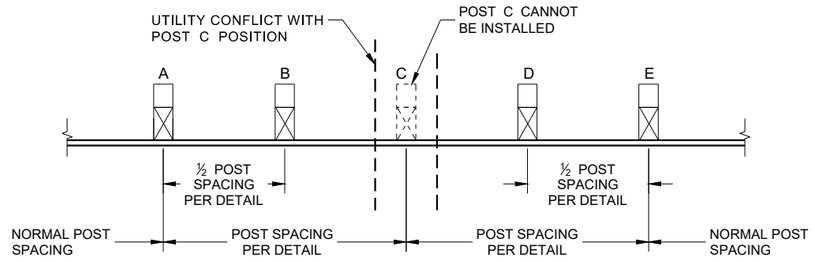
L	T (MIN.)
1 ¼"	1 ⅛"
2"	1 ¾"
10"	4"
14"	4 1⁄16"
18"	4"
21"	4 1⁄16"
25"	4"



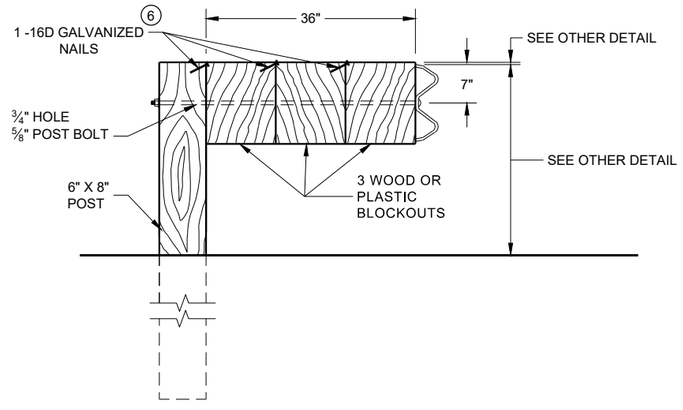
ALTERNATE BOLT HEAD



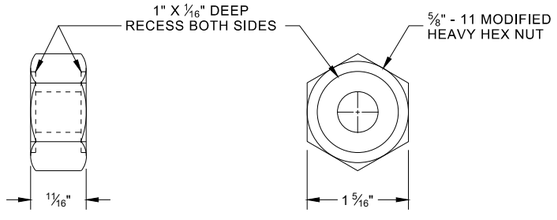
PLAN VIEW
BEAM LAPPING DETAIL



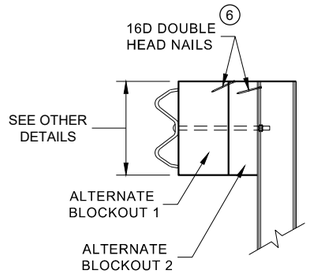
POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION



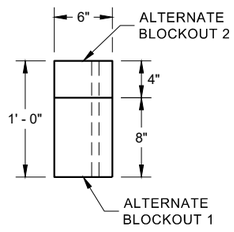
DETAIL FOR 36" BLOCKOUT DEPTH



POST BOLT, SPLICE BOLT AND RECESS NUT



SIDE VIEW



PLAN VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.

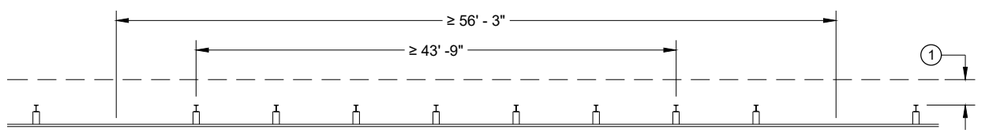
DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

(6) WHEN USING STEEL POST AD WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

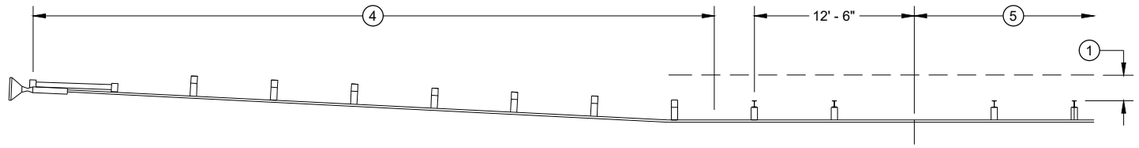
MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

6

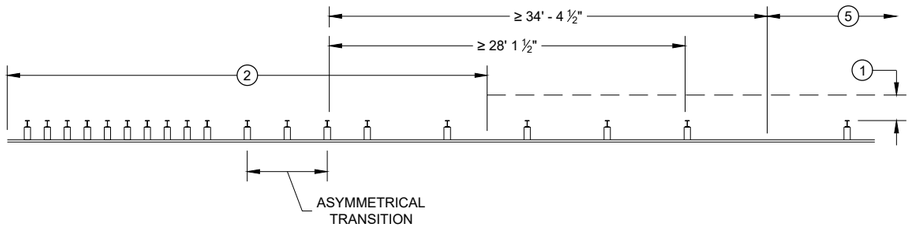
SDD 14B42 - 06c



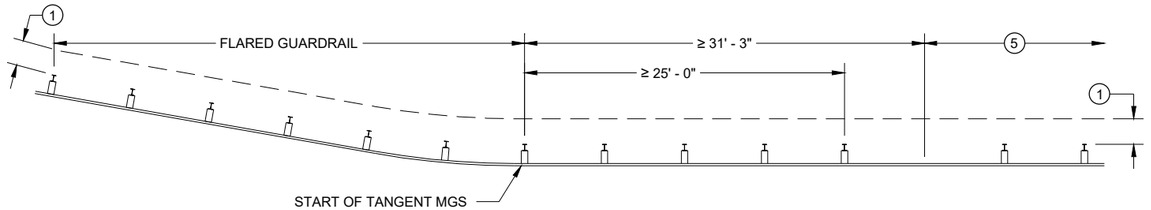
MISSING POST IN NORMAL BEAM GUARD RUN



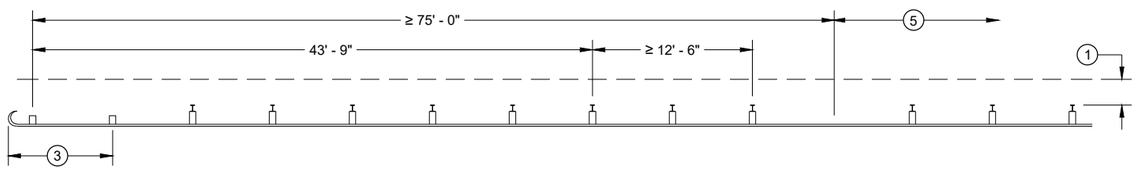
MISSING POST IN NORMAL BEAM GUARD RUN NEAR EAT



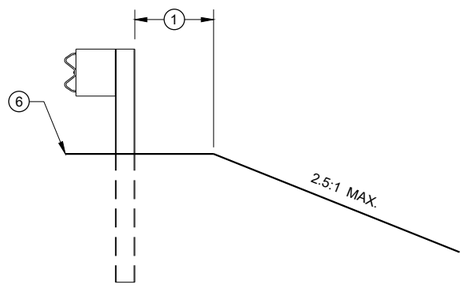
MISSING POST NEAR APPROACH THRIE BEAM TRANSITION



MISSING POST IN NORMAL BEAM GUARD RUN NEAR FLARED BEAM GUARD



MISSING POST IN NORMAL BEAM GUARD RUN NEAR TYPE 2 TERMINAL



CROSS SECTION VIEW

1. MINIMUM OF 2 FEET OF GRADING BEHIND POST.
2. SEE SDD 14B45 FOR MORE DETAILS.
3. SEE SDD 14B47 FOR MORE DETAILS.
4. SEE SDD 14B44 FOR MORE DETAILS.
5. SEE MISSING POST IN NORMAL BEAM GUARD RUN FOR DISTANCE TO NEXT MISSING POST AND AREA FOR WELL DRAINED, COMPACTED SOILS.
6. SEE PLAN FOR SHOULDER DESIGN.

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE UNIT SUPERVISOR

FHWA

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SDD 14B42 - 06d

GENERAL NOTES

(A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL) AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.

(B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED

(C) DIFFERENT MANUFACTURERS REQUIRE DIFFERENT PERFORATED W - BEAM RAIL END PANELS. SEE MANUFACTURER'S INFORMATION.

(D) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF - TAPPING SCREWS. ONE SCREW PER CORNER.

(E) HARDWARE MAY VARY BETWEEN MANUFACTURER. SEE MANUFACTURER'S DRAWING FOR INFORMATION.

DIMENSIONS MAY VARY, MANUFACTURER'S INFORMATION.

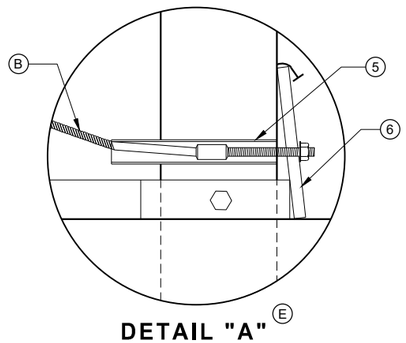
SEE SDD 14B42 FOR MORE INFORMATION.

* DO NOT ATTACH BLOCKOUTS TO POST 1 AND 2.

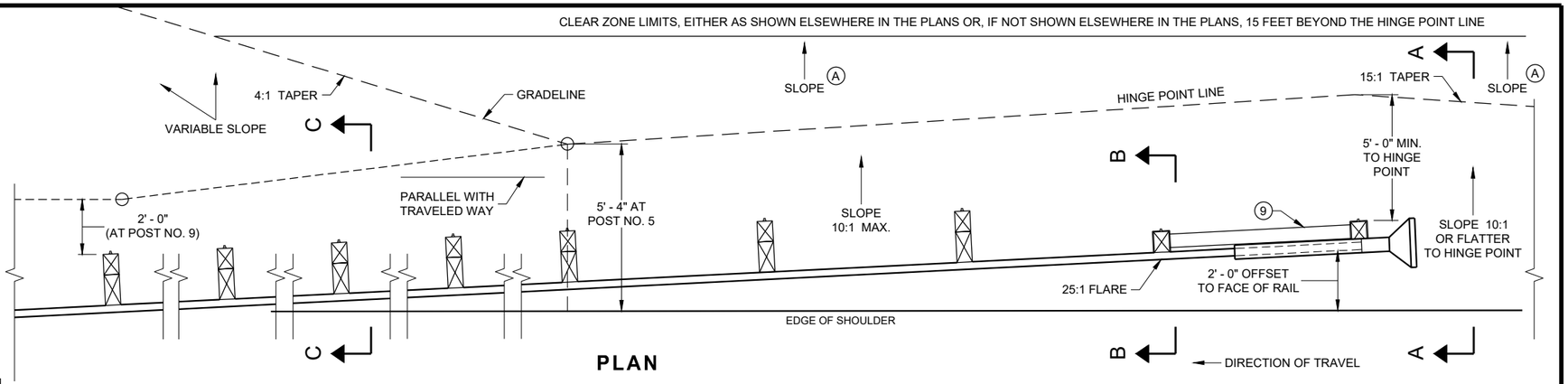
DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

SEE MANUFACTURER'S DRAWING FOR SPLICE LOCATION, HARDWARE DIMENSIONS AND INSTALLATION INSTRUCTIONS.

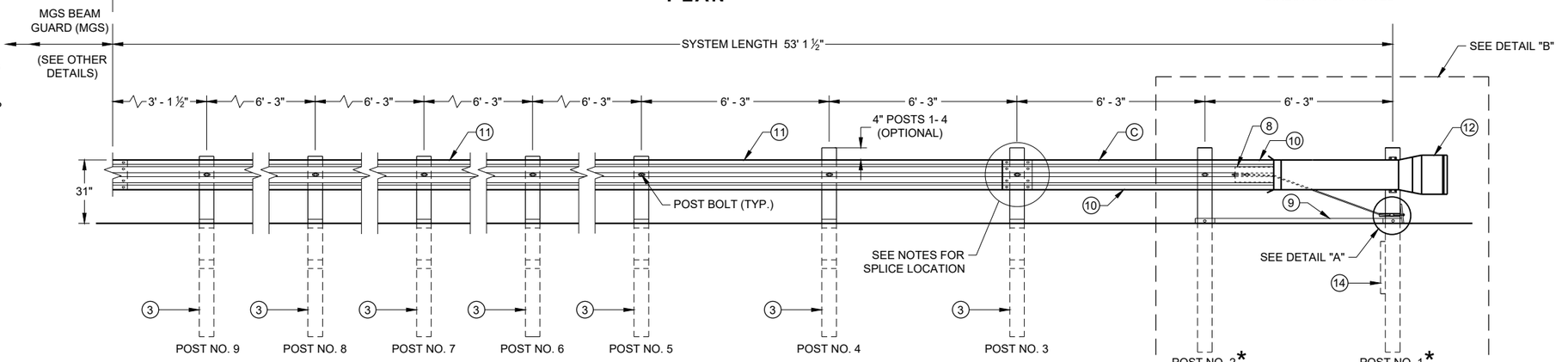
THE CENTER OF THE UPPER 3 ½" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE. WOOD BLOCKS ON POSTS NUMBERED 3 THROUGH 9 MAY BE ADJUSTED UP TO 3" ABOVE THE TOP OF POST.



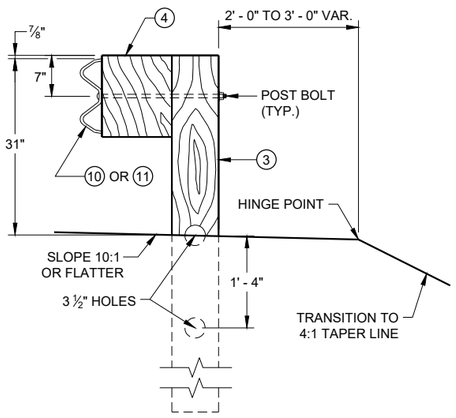
DETAIL "A" (E)



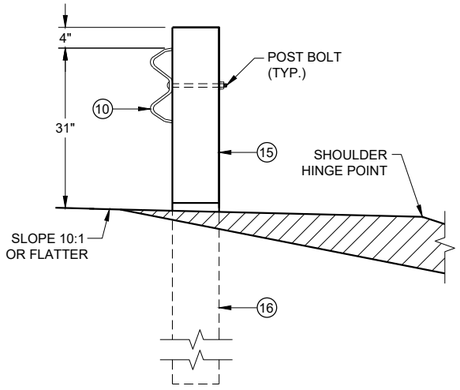
PLAN



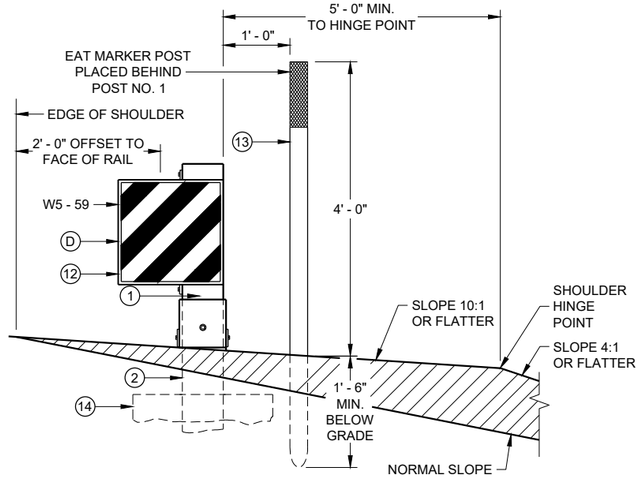
ELEVATION



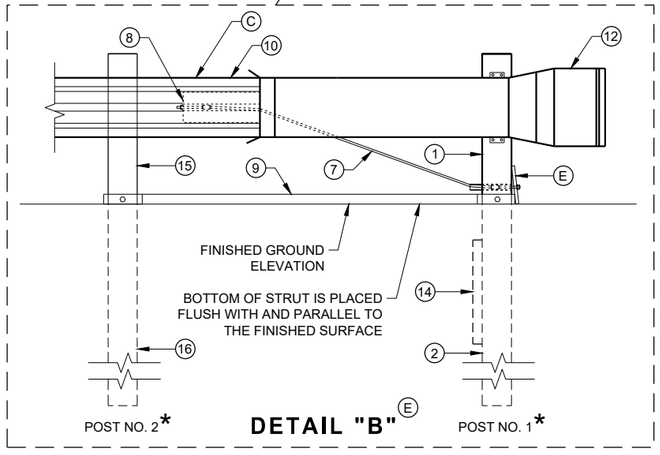
SECTION C - C
TYPICAL AT POST NOS. 3 - 9



SECTION B - B
TYPICAL AT POST NO. 2*



SECTION A - A
TYPICAL AT POST NO. 1*



DETAIL "B" (E)

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL (MGS)

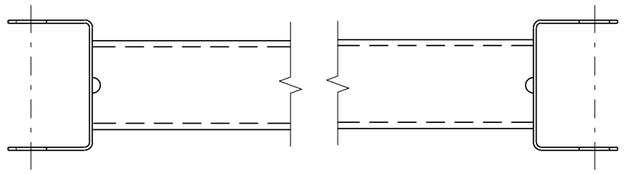
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 14B44 - 04a

SDD 14B44 - 04a

6

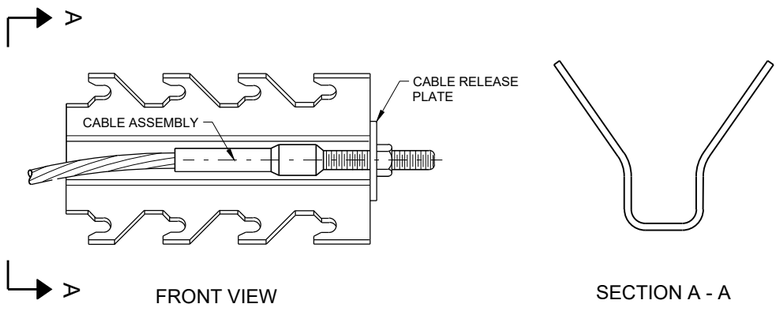
6



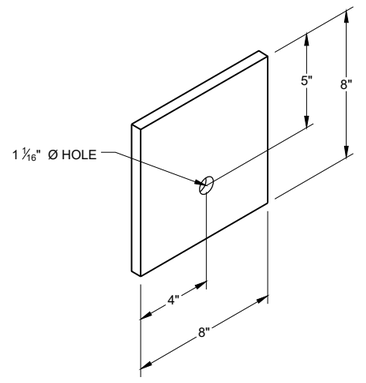
GENERIC GROUND STRUT ⑨ Ⓔ

BILL OF MATERIALS

PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUGACTURER'S DETAILS FOR MORE INFORMATION.
①	UPPER POST NO. 1 6" X 6" TUBE
②	LOWER POST NO. 1
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	IMPACT HEAD
⑬	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)
⑭	SOIL PLATE
⑮	UPPER POST NO. 2
⑯	LOWER POST NO. 2



GENERIC ANCHOR CABLE BOX ⑧ Ⓔ



BEARING PLATE ⑥ Ⓔ

6

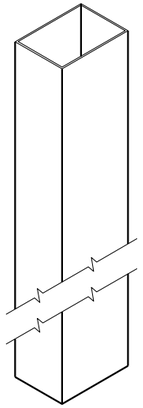
6

SDD 14B44 - 04b

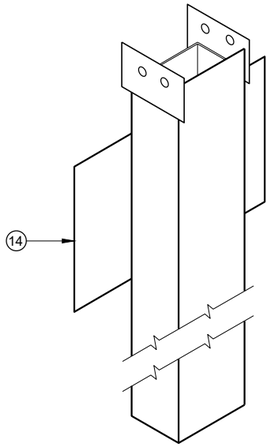
SDD 14B44 - 04b

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)

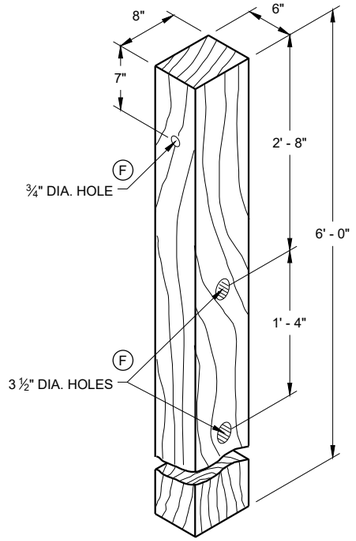
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



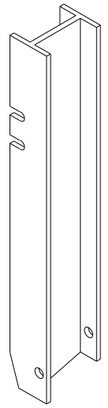
UPPER POST NO. 1 (1) (E)



LOWER POST NO. 1 (2) (E)



WOOD CRT POST (3) (E)
POSTS NUMBER 3-9



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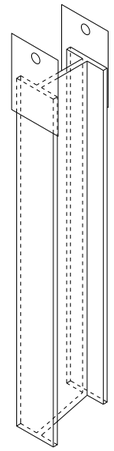
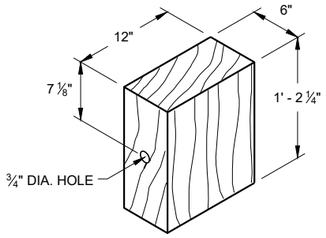
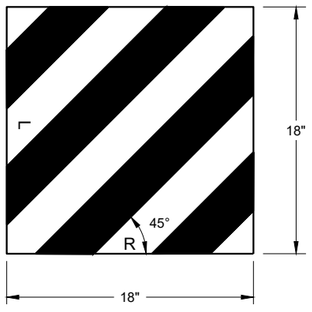


LOWER POST NO. 2 (16) (E)

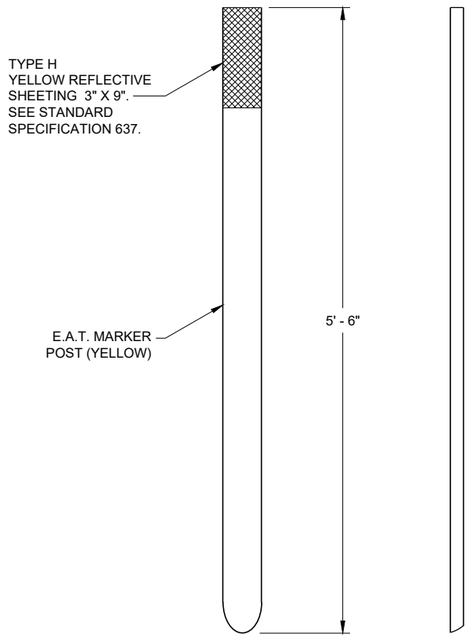


WOOD BLOCKOUT (4)
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2



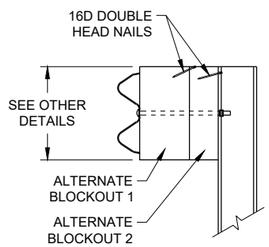
W5 - 59

REFLECTIVE SHEETING DETAIL (E)

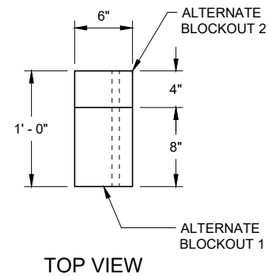


FRONT VIEW SIDE VIEW

E.A.T. MARKER POST (13)



SIDE VIEW



TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)

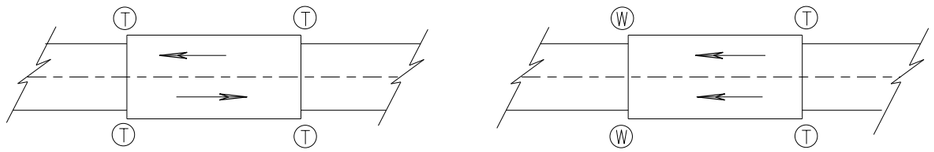
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE UNIT SUPERVISOR

FHWA

6

SDD 14B44 - 04c



TWO WAY TRAFFIC

ONE WAY TRAFFIC

(T) THRIE BEAM CONNECTION

(W) W-BEAM CONNECTION WHEN REQUIRED

TYPICAL LOCATIONS OF THRIE BEAM AND W-BEAM CONNECTIONS TO BRIDGE

GENERAL NOTES

IF ROCK IS ENCOUNTERED, REMOVE ROCK TO FULL DEPTH OF POST PLUS 2½", AND 12" DIAMETER AROUND POST. SEE 14B42 FOR MORE DETAILS.

TRANSITION USES STEEL POSTS ONLY.

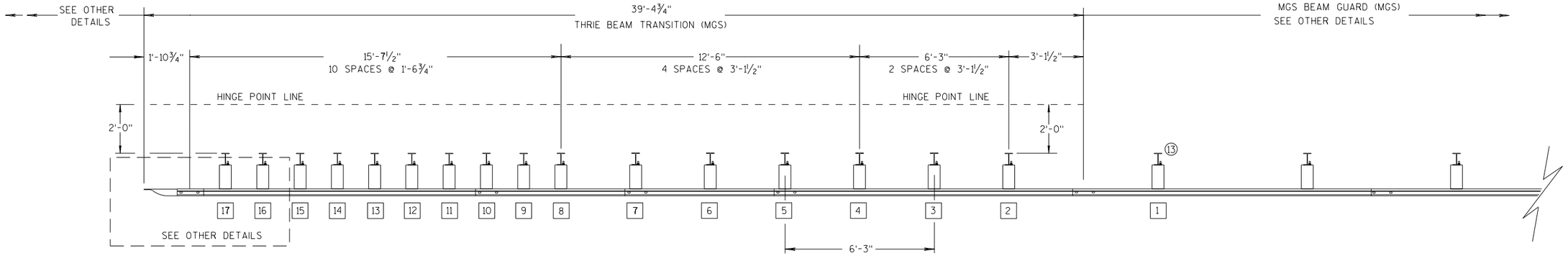
SEE STANDARD DETAIL DRAWING 14 B 42 FOR MORE INFORMATION.

POST 2 THROUGH 17 USES STEEL POST ONLY

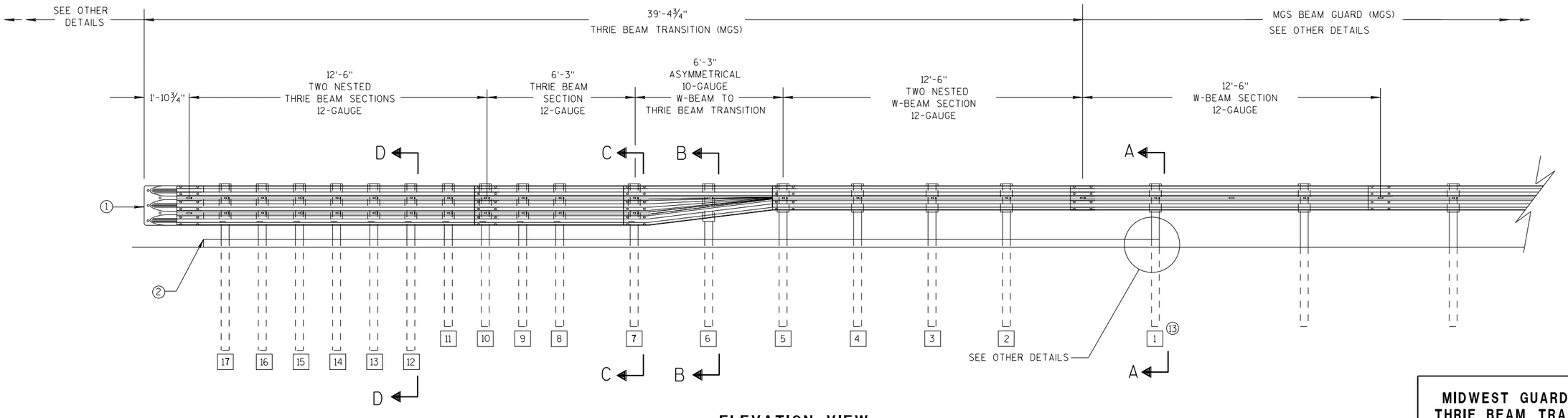
(1) BRIDGE RAILING TYPE "W" DOES NOT REQUIRE A TERMINAL CONNECTOR.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD14B42



PLAN VIEW



ELEVATION VIEW

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 14 B 45-5a

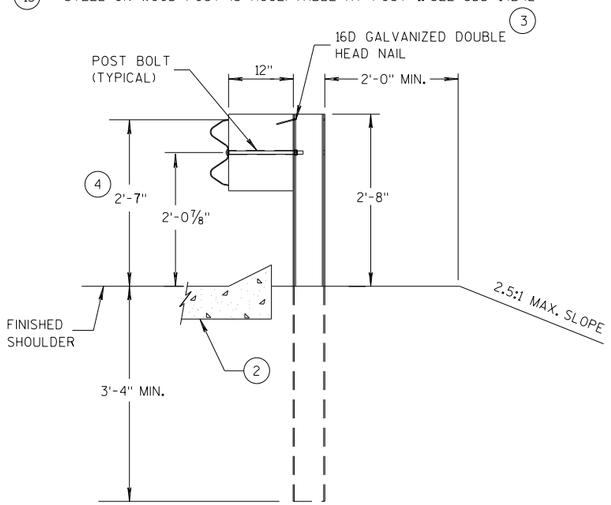
GENERAL NOTES

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 10D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

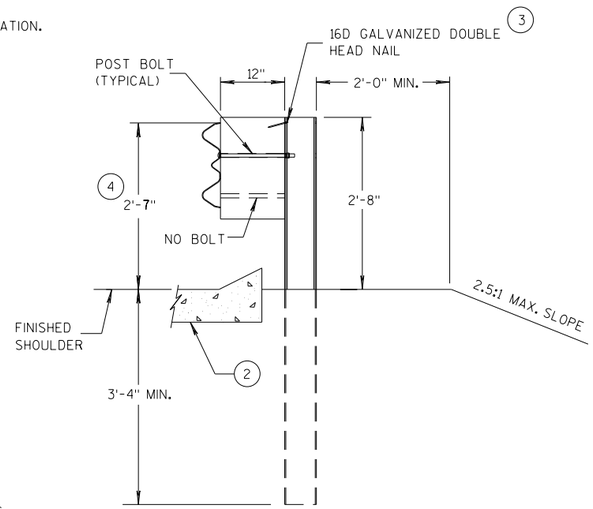
(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".

(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42



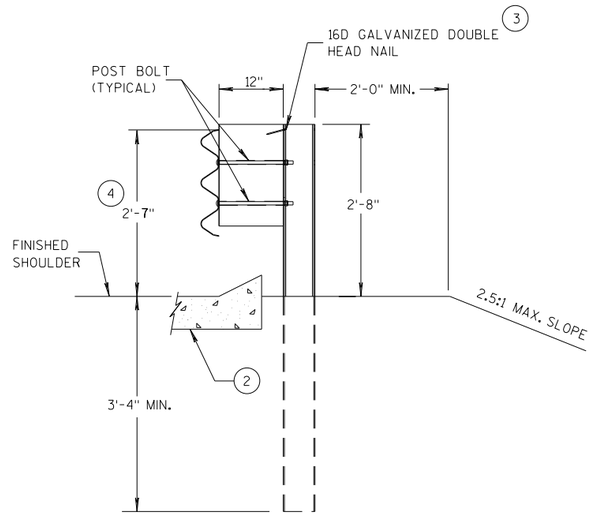
SECTION A-A (13)

POSTS 1-5



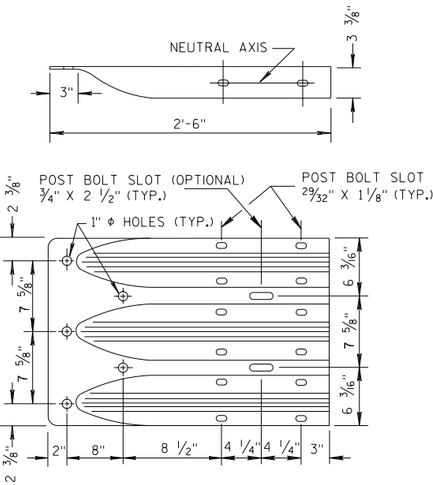
SECTION B-B

POST 6

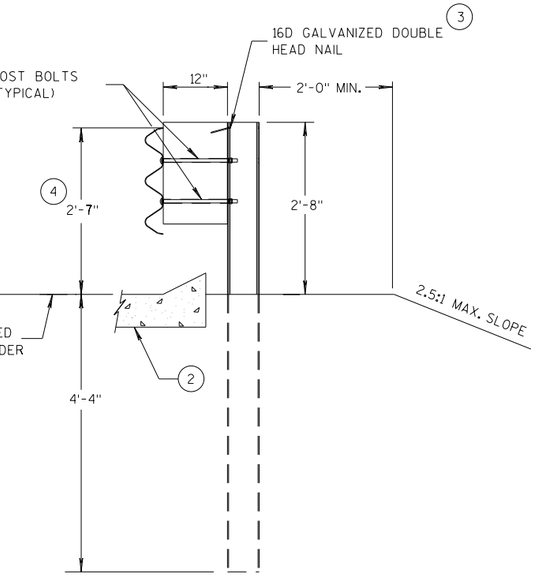


SECTION C-C

POSTS 7-11

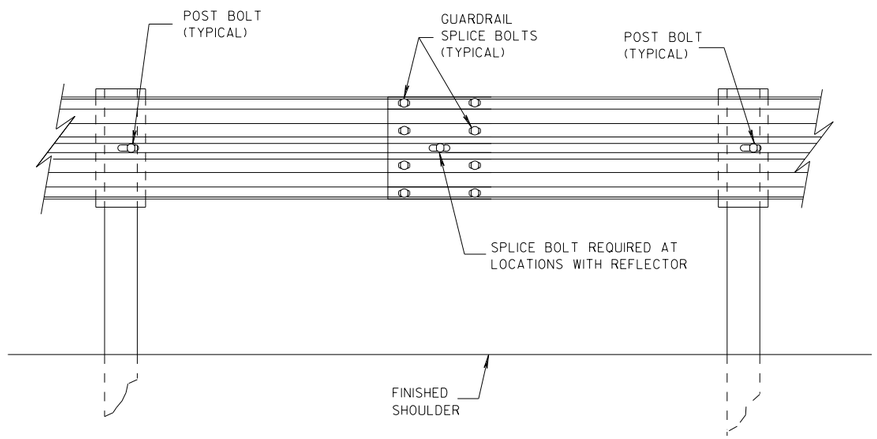


THRIE BEAM TERMINAL CONNECTOR



SECTION D-D

POSTS 12-17



SPLICE DETAIL

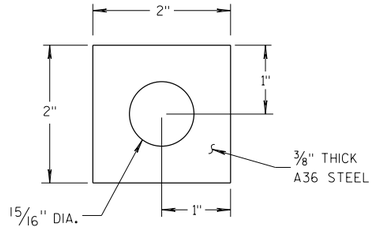
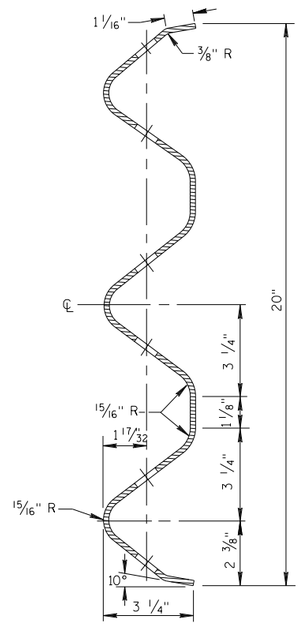


PLATE WASHER DETAIL



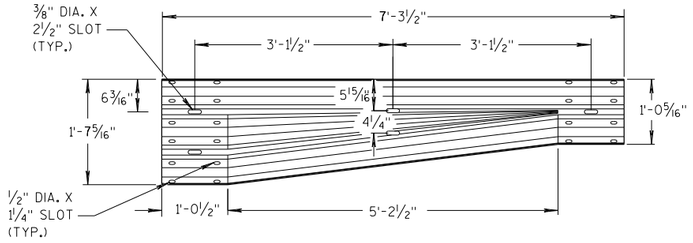
SECTION THRU THRIE BEAM RAIL ELEMENT

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

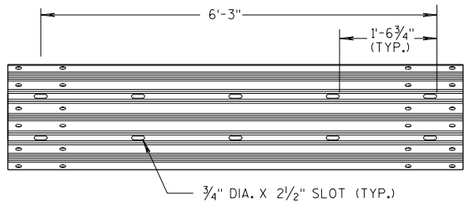
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOF

6

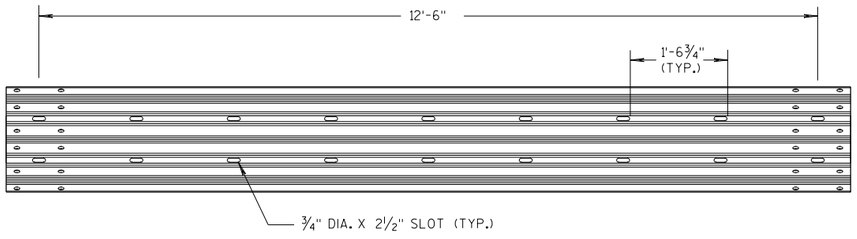
S.D.D. 14 B 45-5b



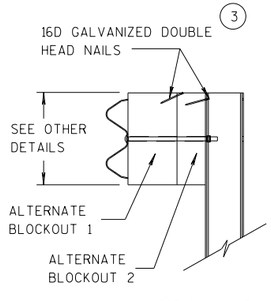
W-BEAM TO THRIE BEAM TRANSITION SECTION



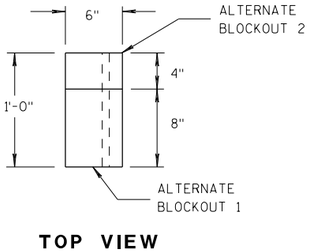
6'-3" THRIE BEAM SECTION



12'-6" THRIE BEAM SECTION

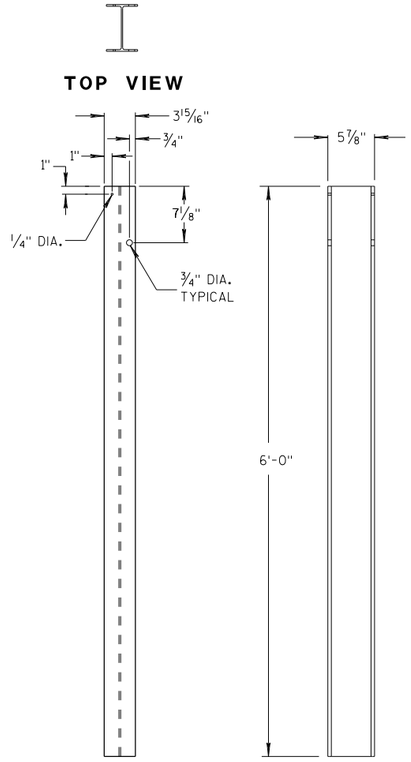


SIDE VIEW



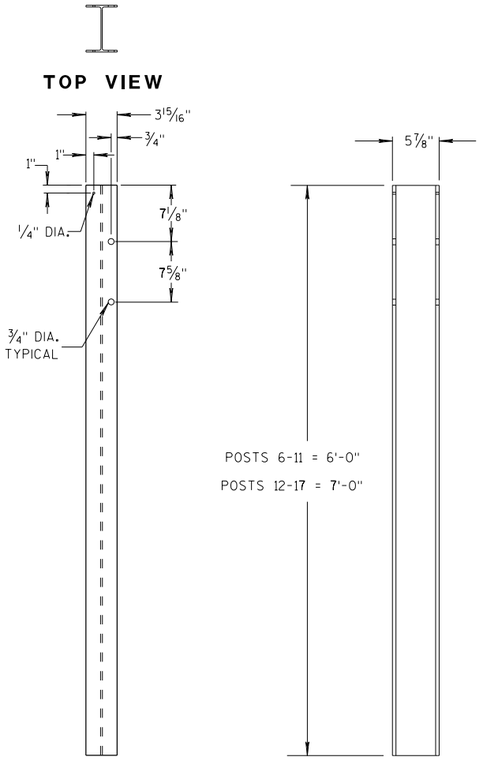
TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL



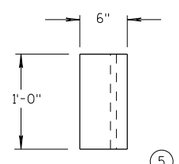
FRONT VIEW SIDE VIEW

STEEL POSTS 1-5 (13)

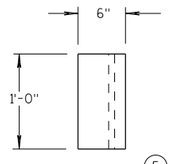


FRONT VIEW SIDE VIEW

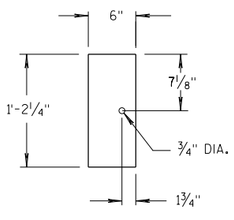
STEEL POSTS 6-17



TOP VIEW (5)

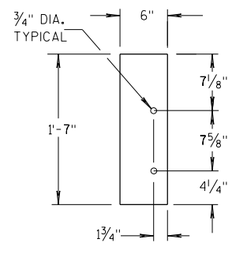


TOP VIEW (5)



FRONT VIEW

BLOCKOUT POSTS 1-5



FRONT VIEW

BLOCKOUT POSTS 6-17

GENERAL NOTES

STEEL POSTS ARE W6X9 OR W6X8.5.

BOLT HOLES FOR POST ARE ON FRONT AND OF SIDE OF POST.

(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

(5) WOOD BLOCKS MAY BE CONSTRUCTED OUT OF 2 WOOD BLOCKS. SEE ALTERNATE WOOD BLOCK DETAIL.

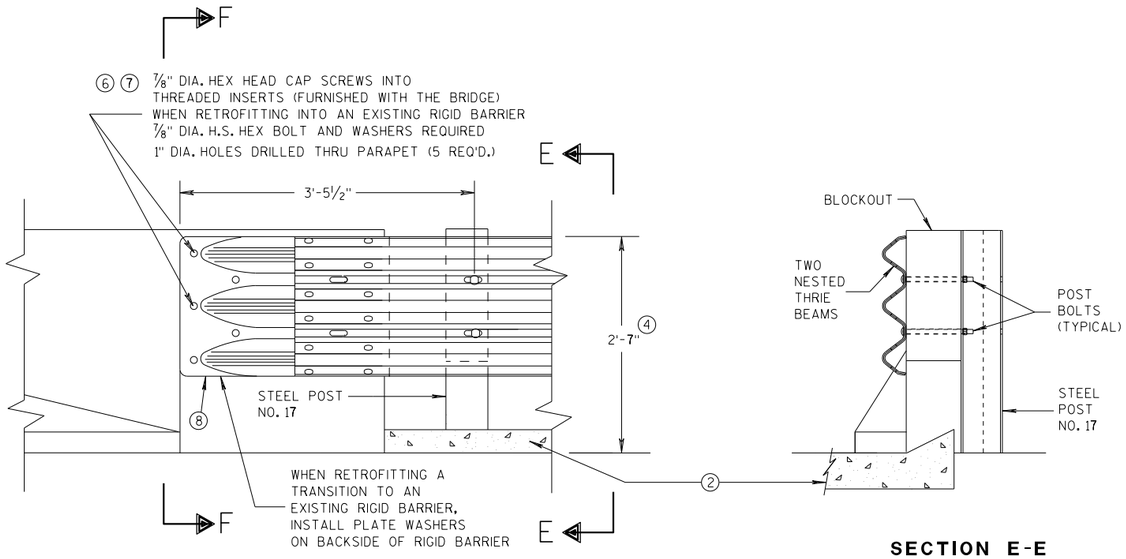
(13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42.

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOF

6

S.D.D. 14 B 45-5c



FRONT VIEW

SECTION E-E

THRIE BEAM CONNECTION TO BRIDGE PARAPET WITH SQUARE ENDS

GENERAL NOTES

THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

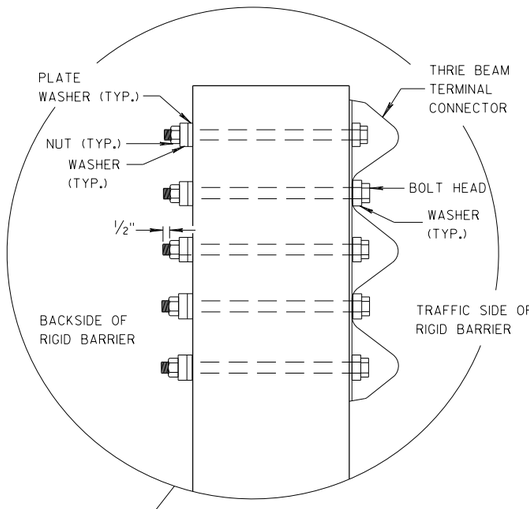
② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

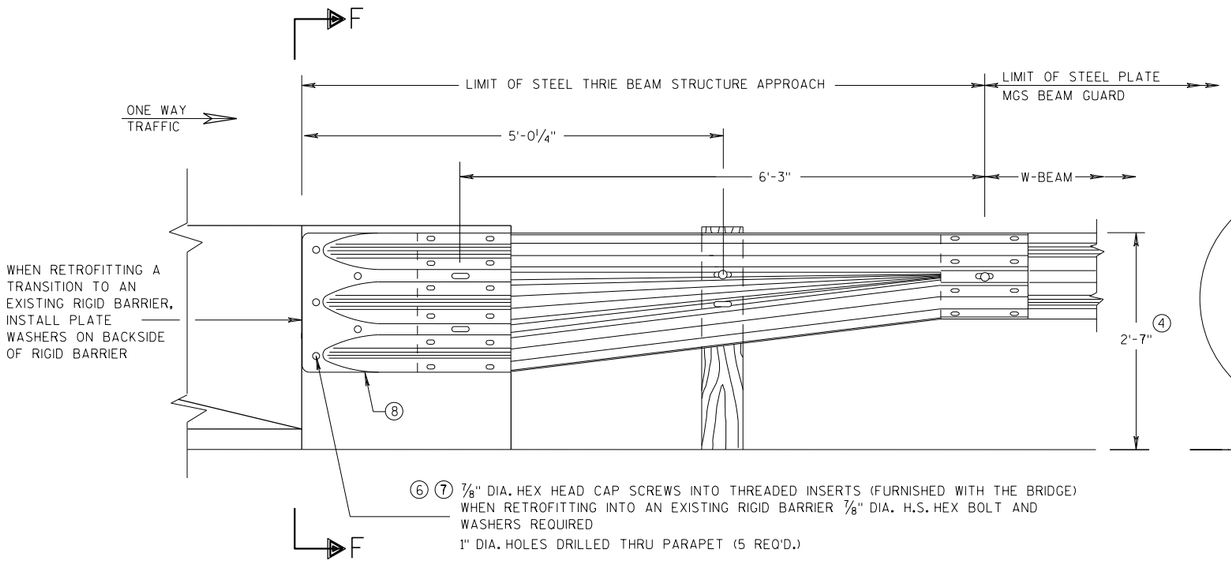
④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

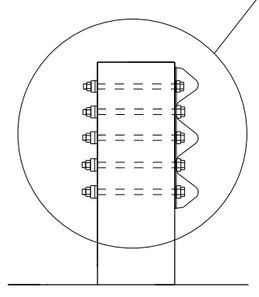
⑦ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/8" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.

⑧ THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".

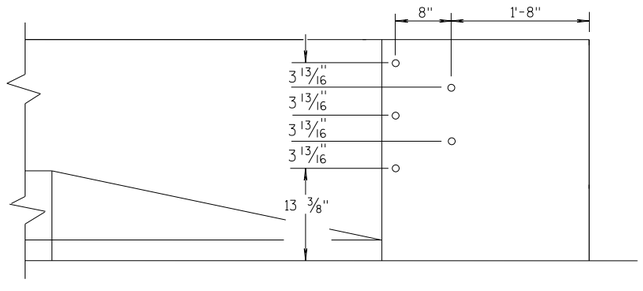




FRONT VIEW



SECTION F-F



DRILL HOLE LOCATION

W BEAM TRANSITION AND CONNECTION TO BRIDGE PARAPETS WITH SQUARE ENDS

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
07/2018 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS D UNIT SUPERVIS
NT
FHWA

6

S.D.D. 14 B 45-5d

6

S.D.D. 14 B 45-5d

GENERAL NOTES

THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

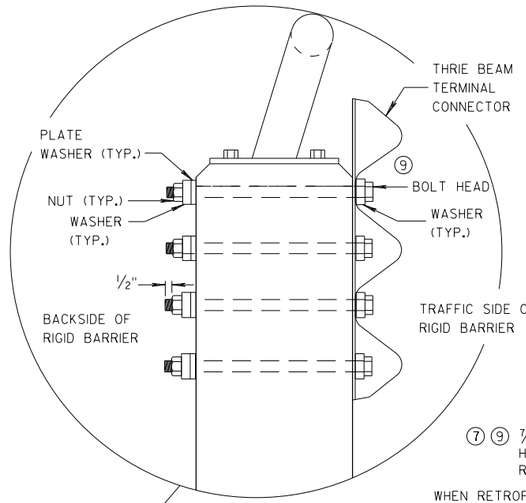
(4) TOLERANCE FOR TOP OF BEAM IS ± 1".

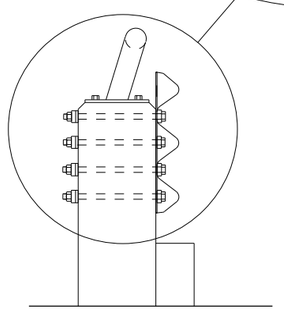
(6) DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

(7) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/8" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.

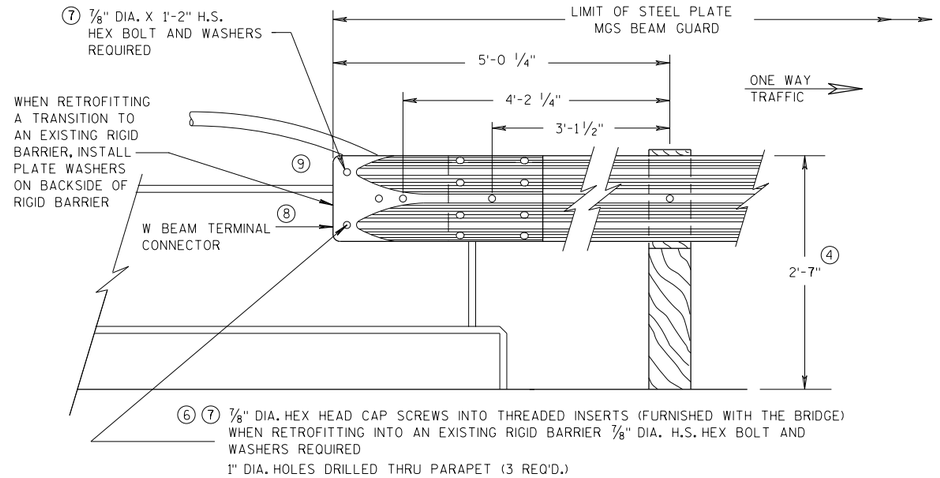
(8) THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".

(9) BOLT, NUT AND WASHERS NOT REQUIRED FOR THIS LOCATION WHEN RETROFITTING AN EXISTING PAPAPET AND THE HOLE IS EITHER ABOVE PARAPET OR WITHIN 4 INCHES OF THE EDGE OF PARAPET.





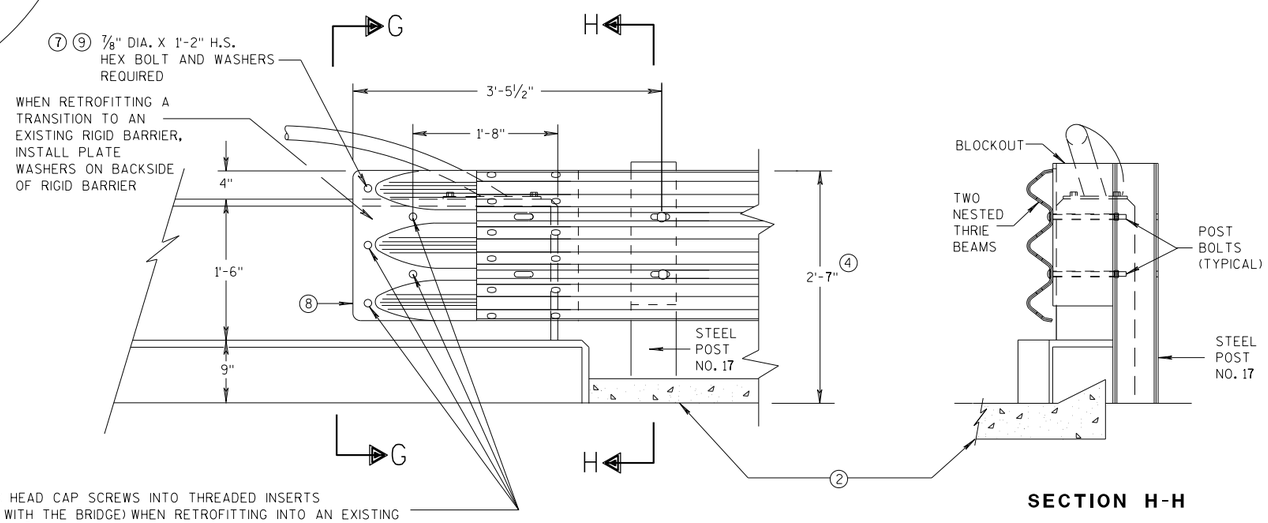
SECTION G-G



FRONT VIEW

W BEAM CONNECTION TO VERTICAL FACE PARAPET

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)



FRONT VIEW

SECTION H-H

THRIE BEAM CONNECTION TO VERTICAL FACED PARAPETS

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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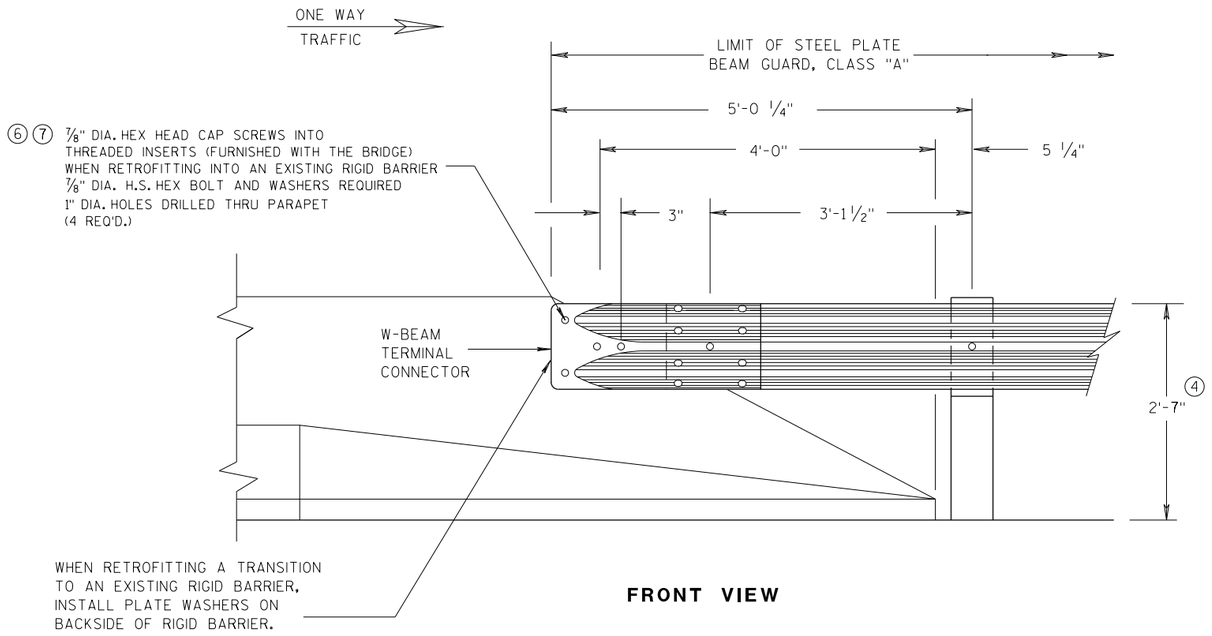

FRONT VIEW

W BEAM CONNECTION TO PARAPETS WITH SLOPED ENDS

(USE ONLY AT TRAFFIC EXIT END OF ONE WAY BRIDGE)

GENERAL NOTES

② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

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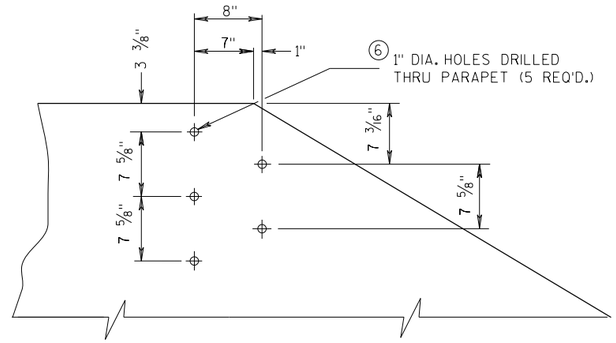
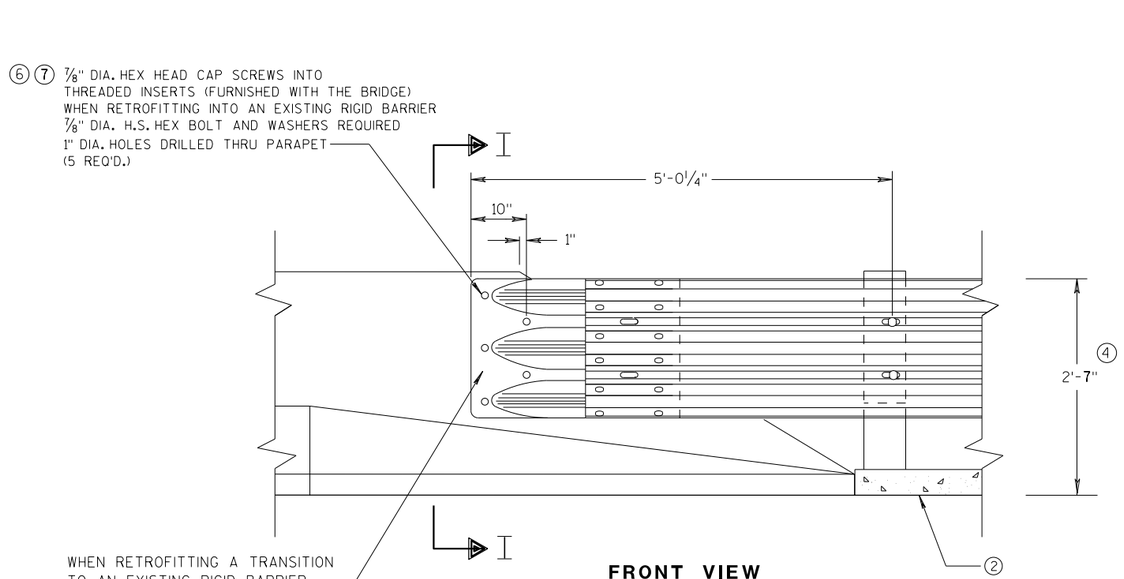
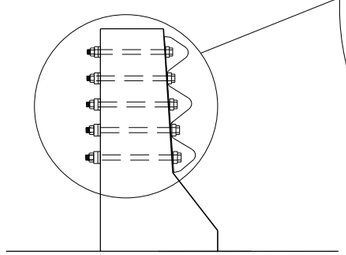


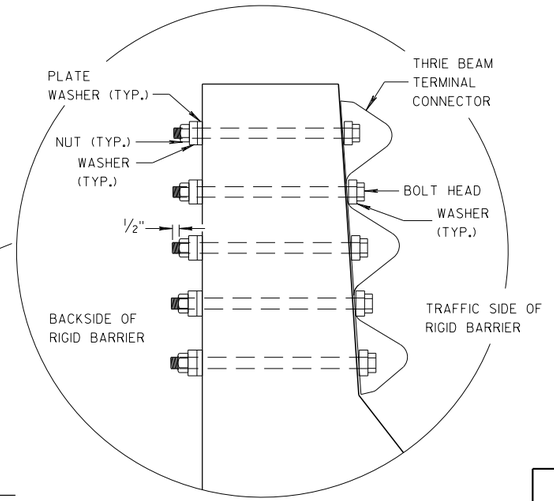
DRILL HOLE LOCATION AND PATTERN
FOR THRIE BEAM CONNECTION



FRONT VIEW



SECTION I-I



THRIE BEAM CONNECTION TO BRIDGE PARAPETS WITH SLOPED ENDS

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

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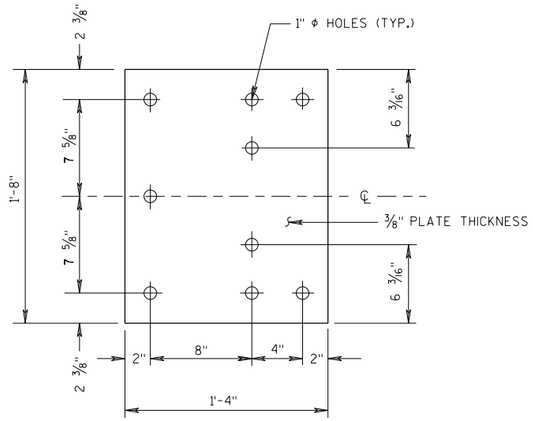
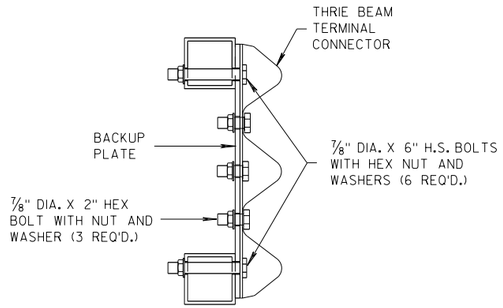
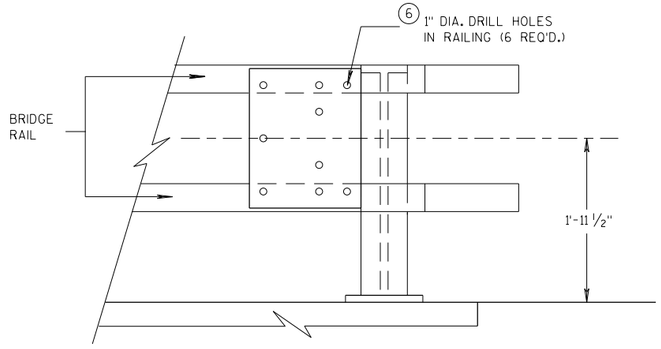


BACK-UP PLATE DETAIL



SECTION J-J

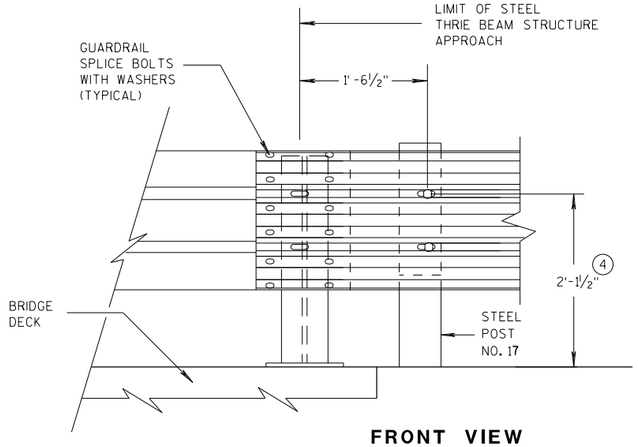


BACK-UP PLATE MOUNTING ONTO BRIDGE RAILING

GENERAL NOTES

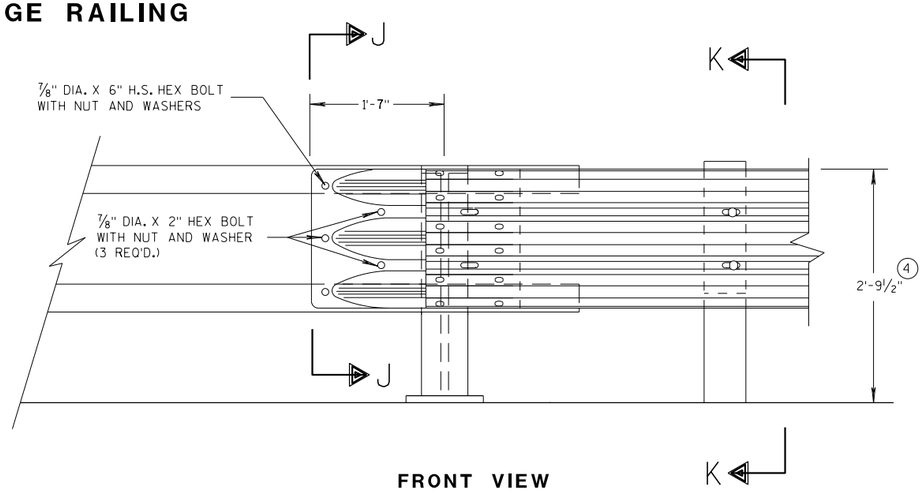
④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING HOLES THROUGH THE PAPRPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

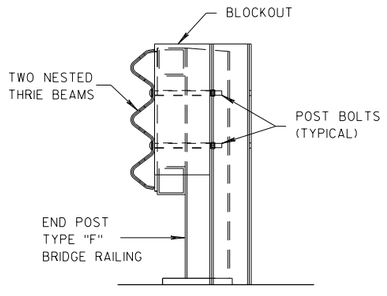


FRONT VIEW

THRIE BEAM CONNECTION TO STEEL RAILING TYPE "W"



FRONT VIEW



SECTION K-K

THRIE BEAM CONNECTION TO TUBULAR RAILING TYPE "F"

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

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DEPARTMENT OF TRANSPORTATION

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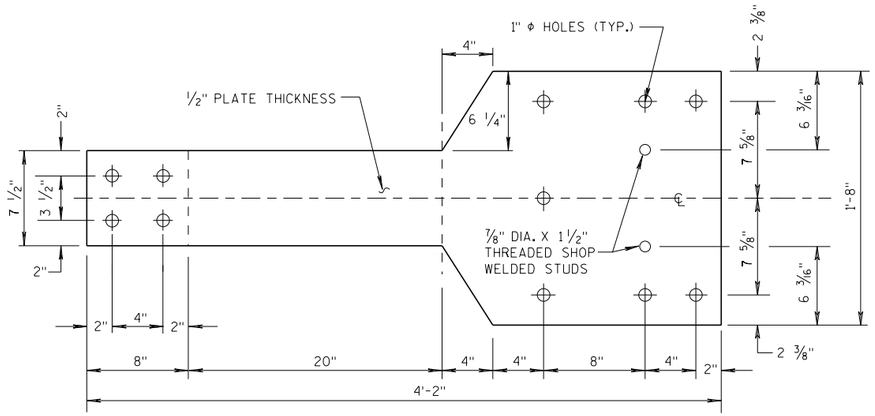
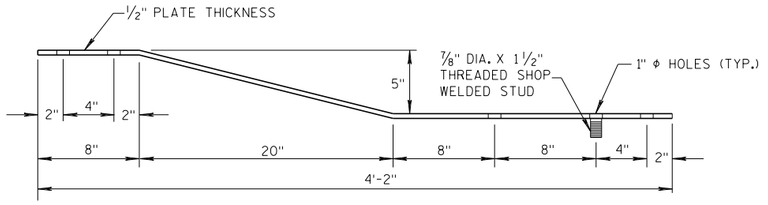
GENERAL NOTES

(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".

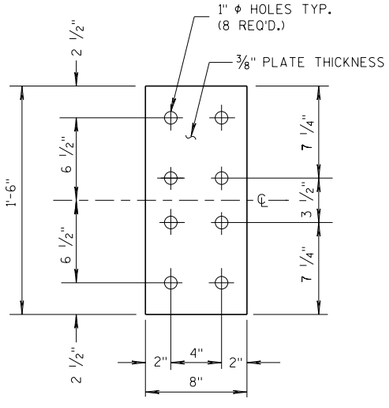


FRONT VIEW



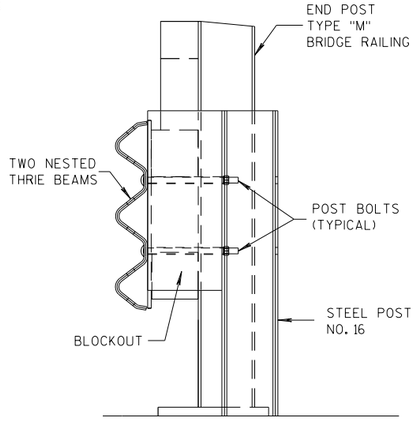
PLAN VIEW

BACK-UP PLATE DETAIL, TYPE "M"

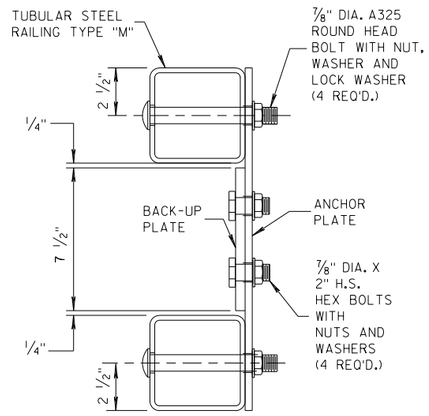


FRONT VIEW

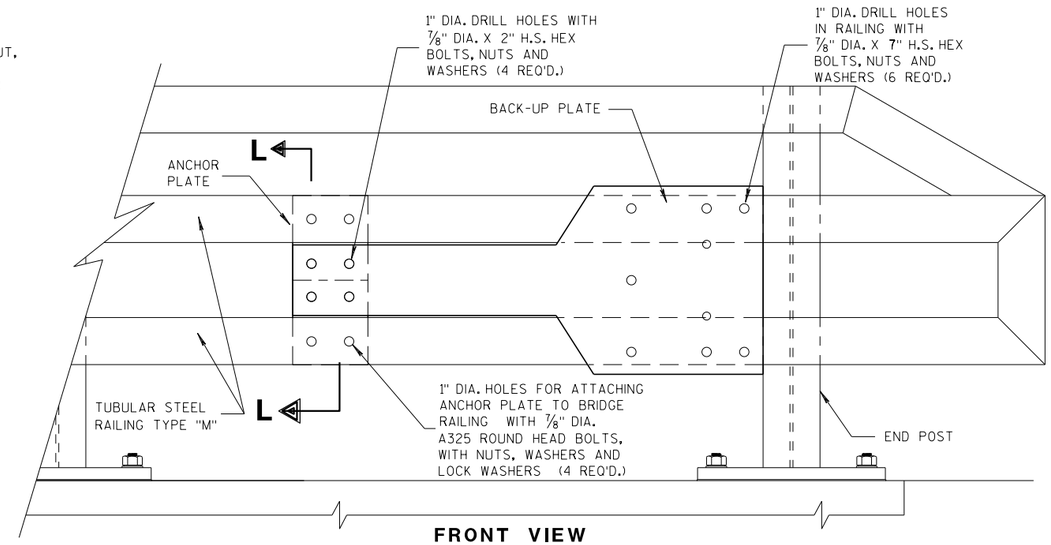
ANCHOR PLATE DETAIL, TYPE "M"



SECTION M-M

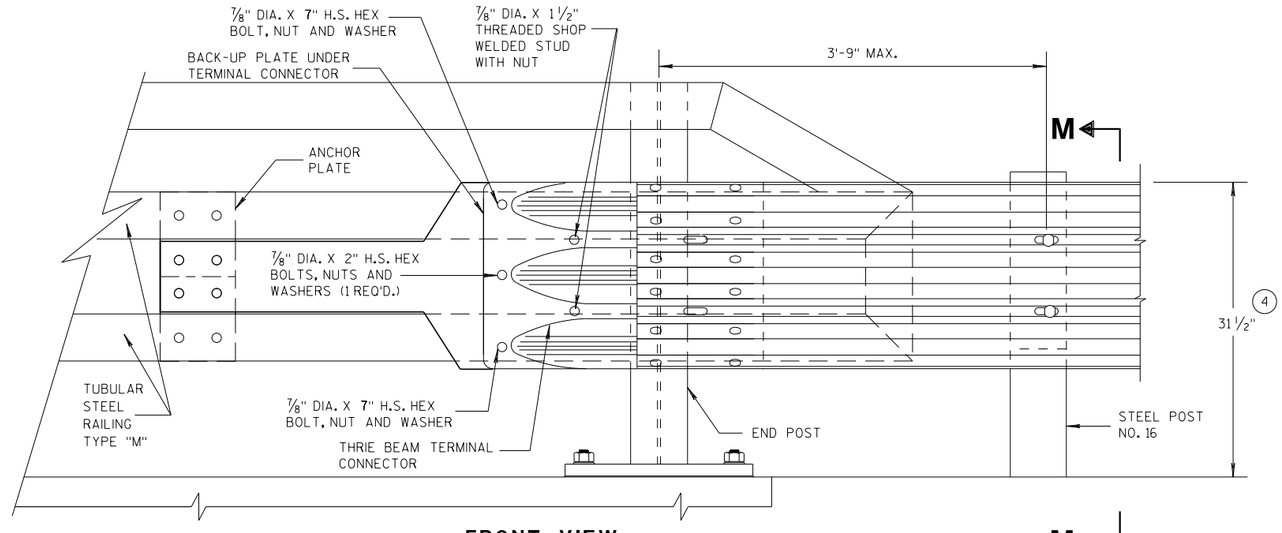


SECTION L-L

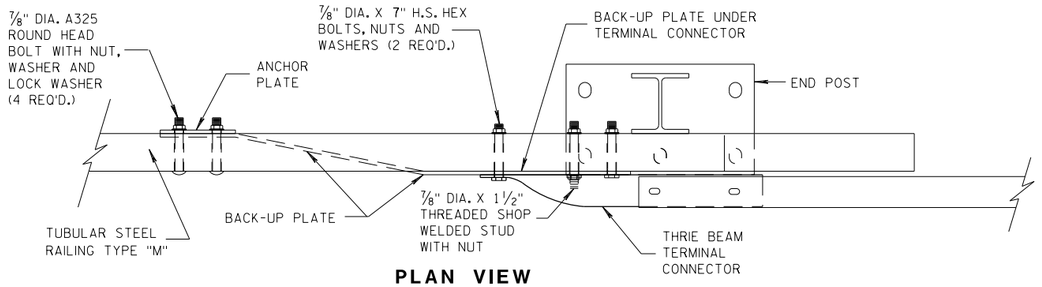


FRONT VIEW

ANCHOR AND BACK-UP PLATE MOUNTING TO BRIDGE RAILING, TYPE "M"



FRONT VIEW



PLAN VIEW

THRIE BEAM CONNECTION TO TUBULAR RAILING, TYPE "M"

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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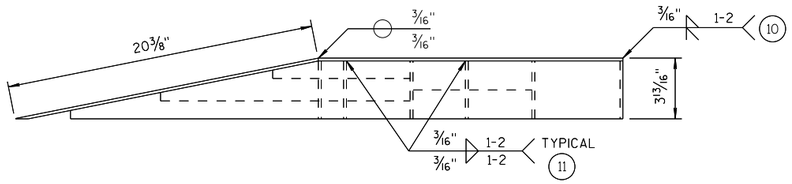

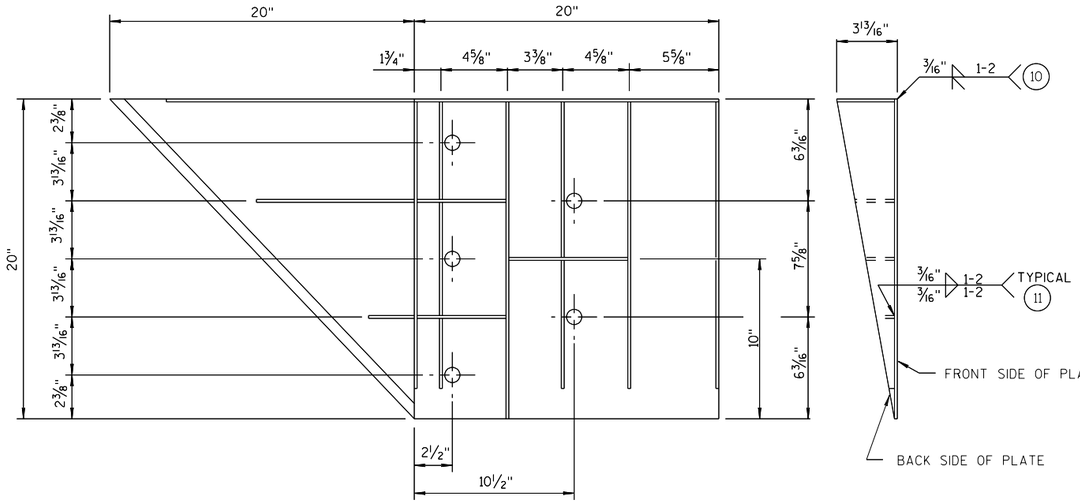

WELDING INSTRUCTION

(VIEWED FROM BACK SIDE OF PLATE)

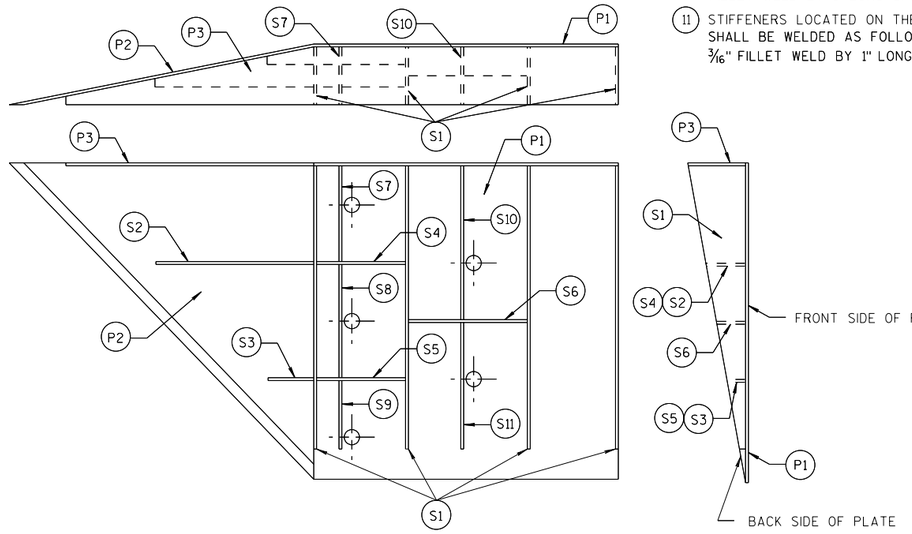

PLATE AND STIFFENER IDENTIFICATION

(VIEWED FROM BACK SIDE OF PLATE)

GENERAL NOTES

COVER PLATE PANELS ARE 3/16" THICK.

ALL STIFFENERS ARE 1/4" THICK.

CONNECTOR PLATE SHALL BE FABRICATED FROM ASTM GRADE A36 STEEL AND GALVANIZED.

FOR GALVANIZED REQUIREMENTS, SEE SECTION 614 OF THE STANDARD SPECIFICATIONS.

ALL HOLE DIAMETERS SHALL BE 1".

FOR OPPOSITE SIDE INSTALLATION MIRROR DRAWINGS.

(10) STIFFENERS LOCATED AT THE OUTSIDE EDGES OF THE COVER PLATES SHALL BE WELDED AS FOLLOWS:
SINGLE BEVEL GROOVE WELD ON EXTERNAL SIDES AND 3/16" FILLET WELD BY 1" LONG SPACED AT 2" ON INTERNAL SIDES.

(11) STIFFENERS LOCATED ON THE INSIDE OF THE COVER PLATE SHALL BE WELDED AS FOLLOWS:
3/16" FILLET WELD BY 1" LONG SPACED AT 2".

CONNECTOR PLATE DIMENSION (PER ASSEMBLY)

PLATE	QUANTITY	SHAPE	SIZE (A x B x C x D)	THICKNESS
P1	1		20" x 20"	3/16"
P2	1		20" x 20" x 28 9/16"	3/16"
P3	1		39" x 3 5/8" x 20" x 19 5/16"	3/16"
S1	4		18 7/16" x 3 5/8" x 18 3/4"	1/4"
S2	1		10 1/4" x 2 7/16" x 10 3/8" x 1/2"	1/4"
S3	1		3" x 1 1/16" x 3 1/8" x 1/2"	1/4"
S4	1		6 1/8" x 2 7/16"	1/4"
S5	1		6 1/8" x 1 1/16"	1/4"
S6	1		7 3/4" x 1 3/4"	1/4"
S7	1		2 9/16" x 6" x 3 5/8" x 5 7/8"	1/4"
S8	1		1 5/32" x 7 1/2" x 2 1/2" x 7 3/8"	1/4"
S9	1		6 1/16" x 6 3/16" x 1 3/32"	1/4"
S10	1		1 7/8" x 9 7/8" x 3 5/8" x 9 11/16"	1/4"
S11	1		8 1/2" x 8 3/4" x 1 13/16"	1/4"

SINGLE SLOPE CONNECTION PLATE

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

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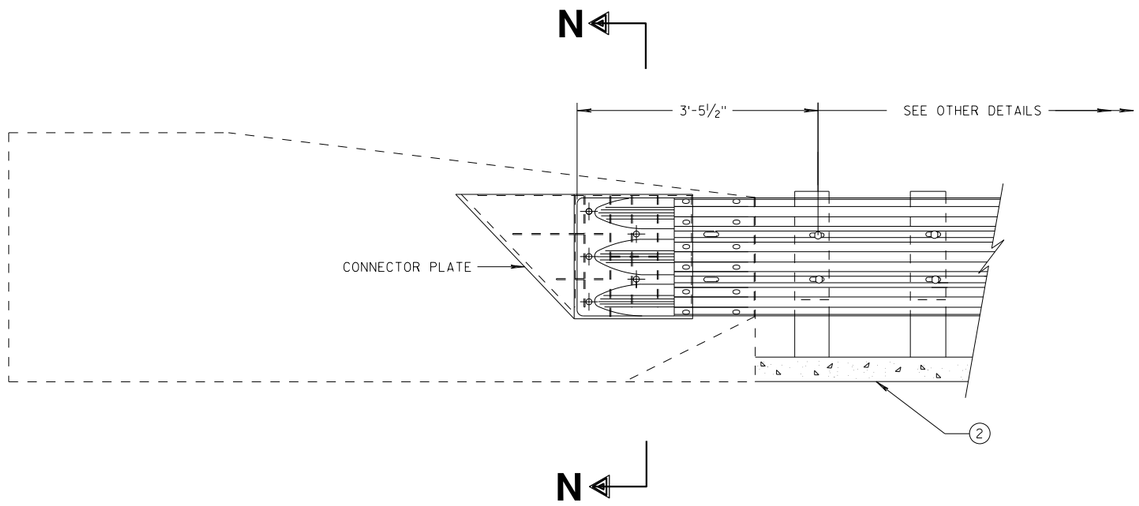
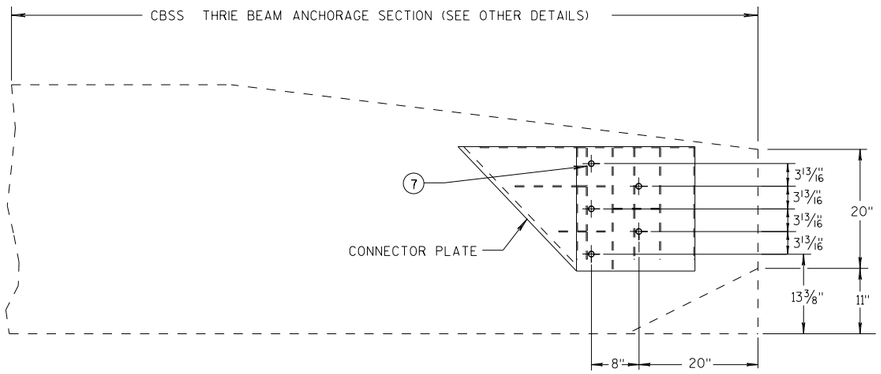


THRIE BEAM CONNECTION TO SINGLE SLOPE BARRIER



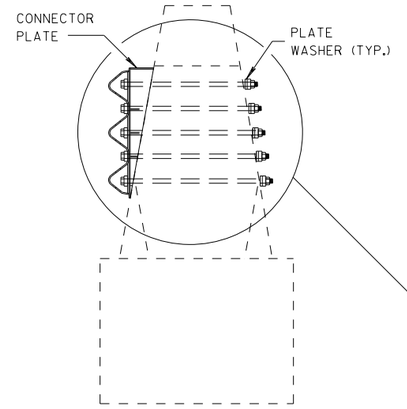
SINGLE SLOPE CONNECTION PLATE PLACEMENT

GENERAL NOTES

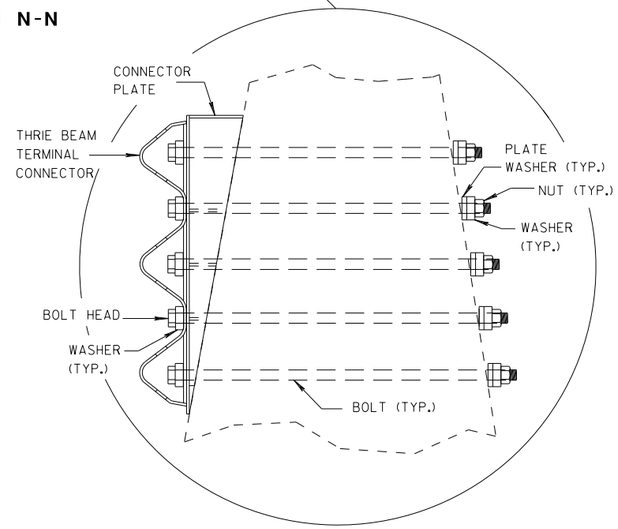
CONNECTOR PLATE, DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(7) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X ⅝" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.



SECTION N-N



MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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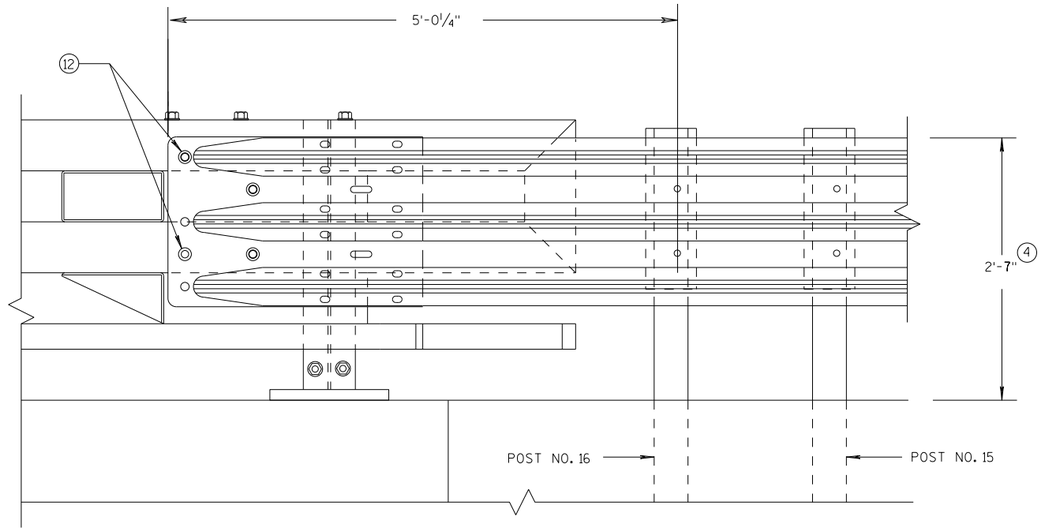
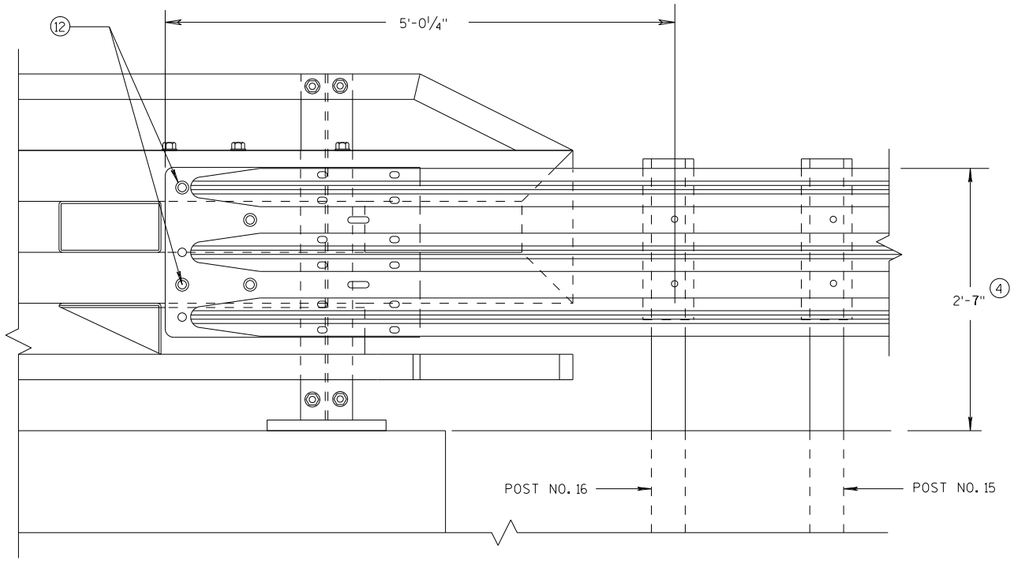


ELEVATION OF DETAIL AT NY3 END POST

THRIE BEAM RAIL ATTACHMENT



ELEVATION OF DETAIL AT NY4 END POST

THRIE BEAM RAIL ATTACHMENT

GENERAL NOTES

(4) TOLERANCE FOR TOP OF BEAM IS ± 1".

(12) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND 1/2-INCH BEYOND NUT.

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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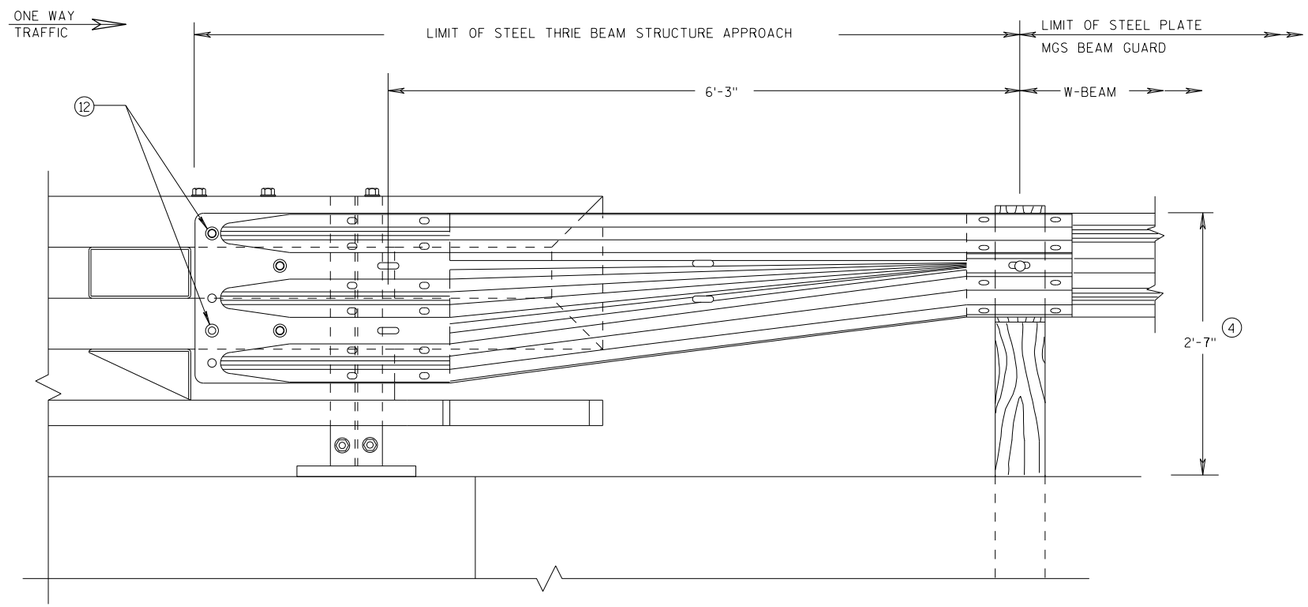


FRONT VIEW

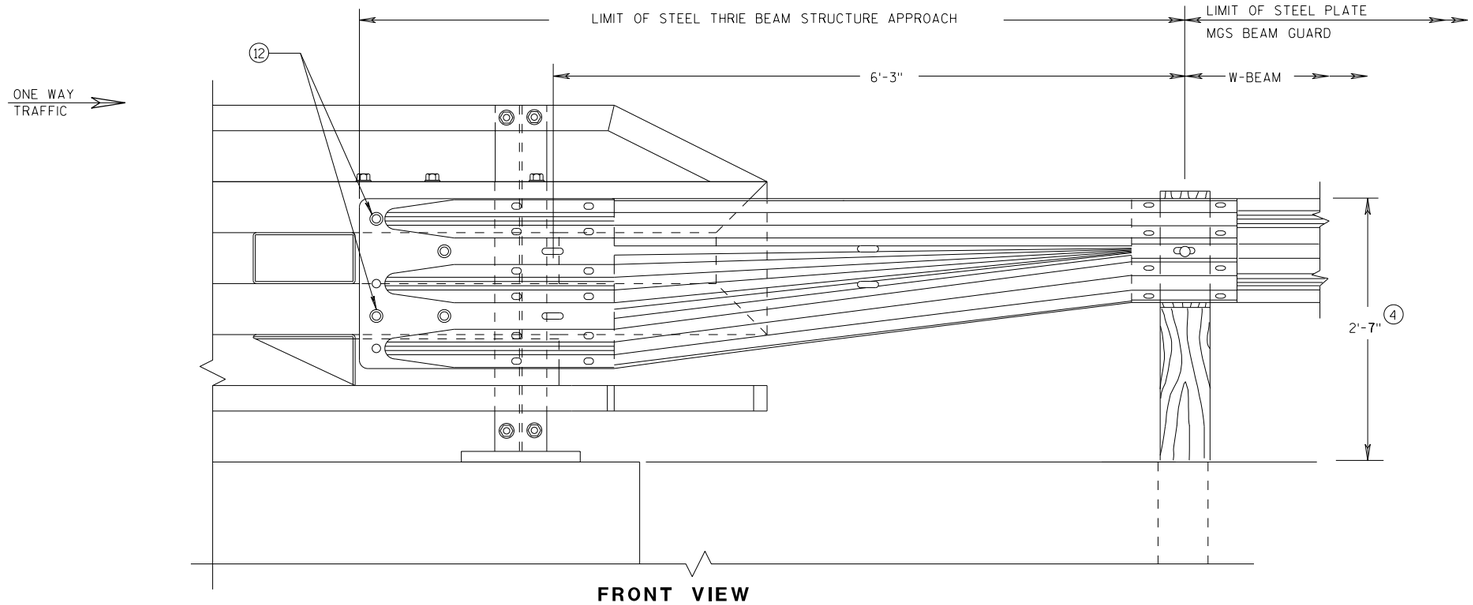
W BEAM TRANSITION AND CONNECTION TO BRIDGE RAILING TYPE "NY3"

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

GENERAL NOTES

(4) TOLERANCE FOR TOP OF BEAM IS ± 1".

(12) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND 1/2-INCH BEYOND NUT.



FRONT VIEW

W BEAM TRANSITION AND CONNECTION TO BRIDGE RAILING TYPE "NY4"

(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
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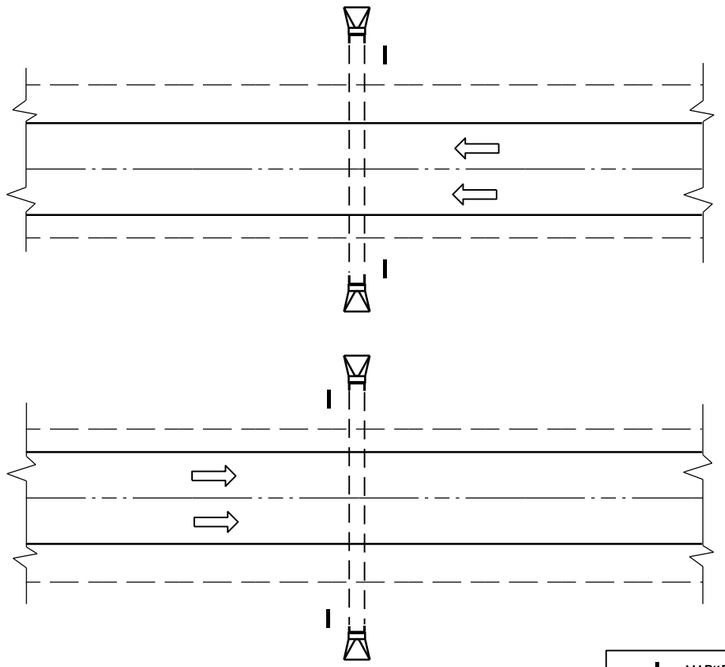
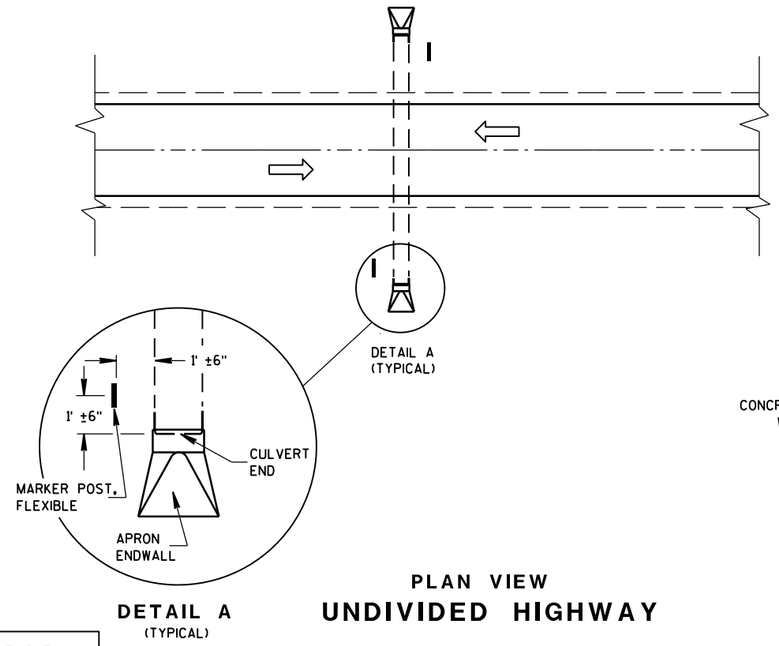
S.D.D. 14 B 45-5L



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

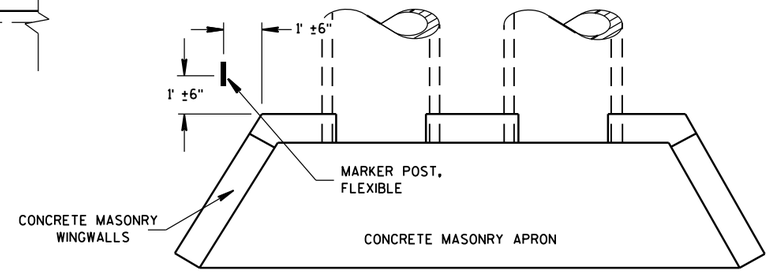
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

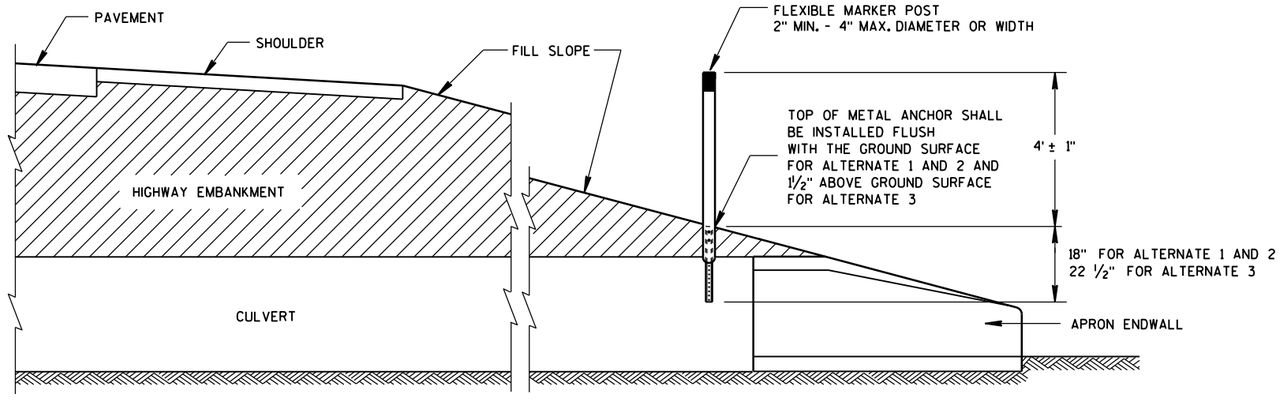
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST FOR CULVERT END

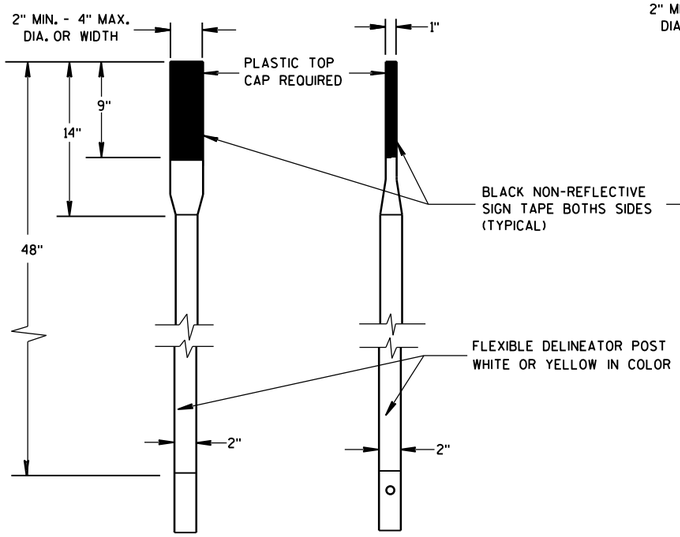
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

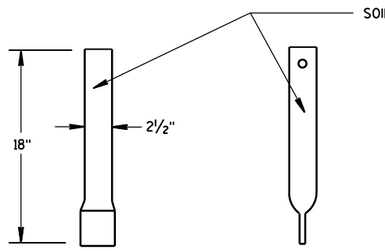
6

S.D.D. 15 A 3-2a

6

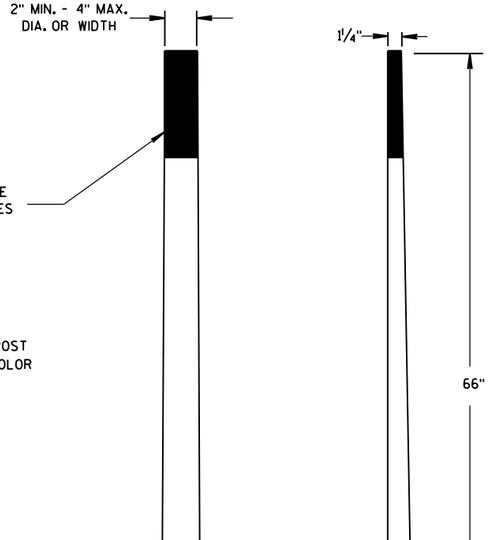
S.D.D. 15 A 3-2a





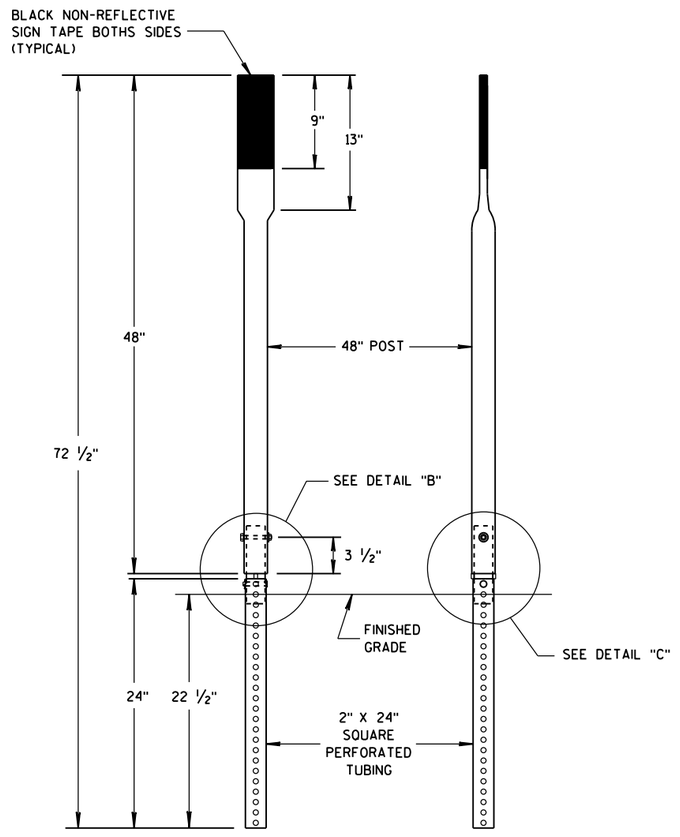
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

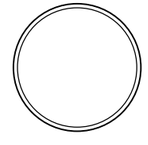
ALTERNATE 2



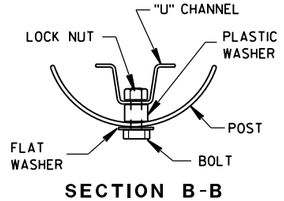
FRONT VIEW SIDE VIEW

ALTERNATE 3

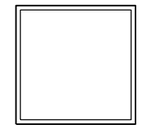
FLEXIBLE MARKER POSTS



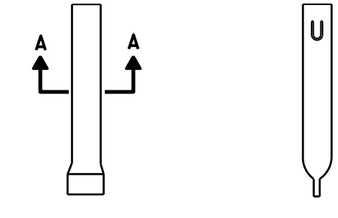
SECTION A-A



SECTION B-B

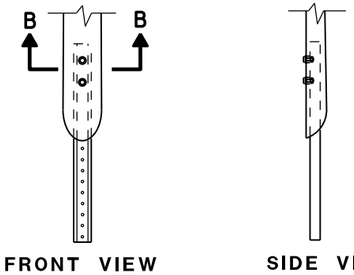


SECTION C-C



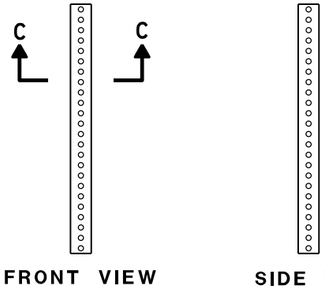
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

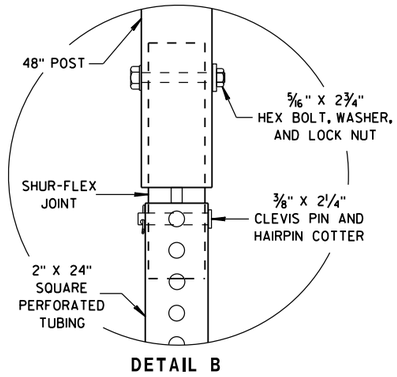
ALTERNATE 2



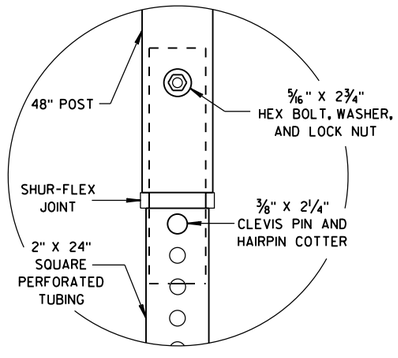
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS



DETAIL B



DETAIL C

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

6

S.D.D. 15 A 3-2b

6

S.D.D. 15 A 3-2b

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

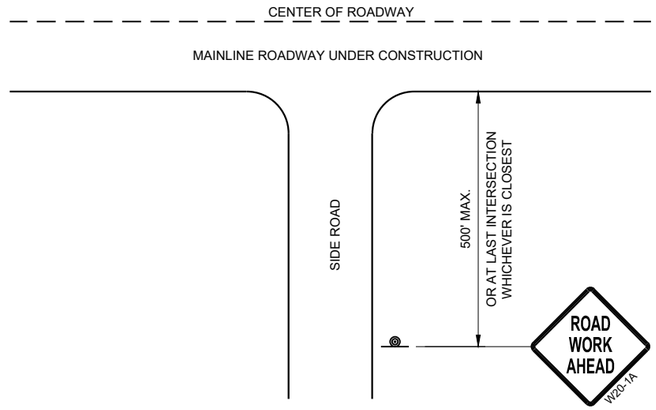
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS

** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



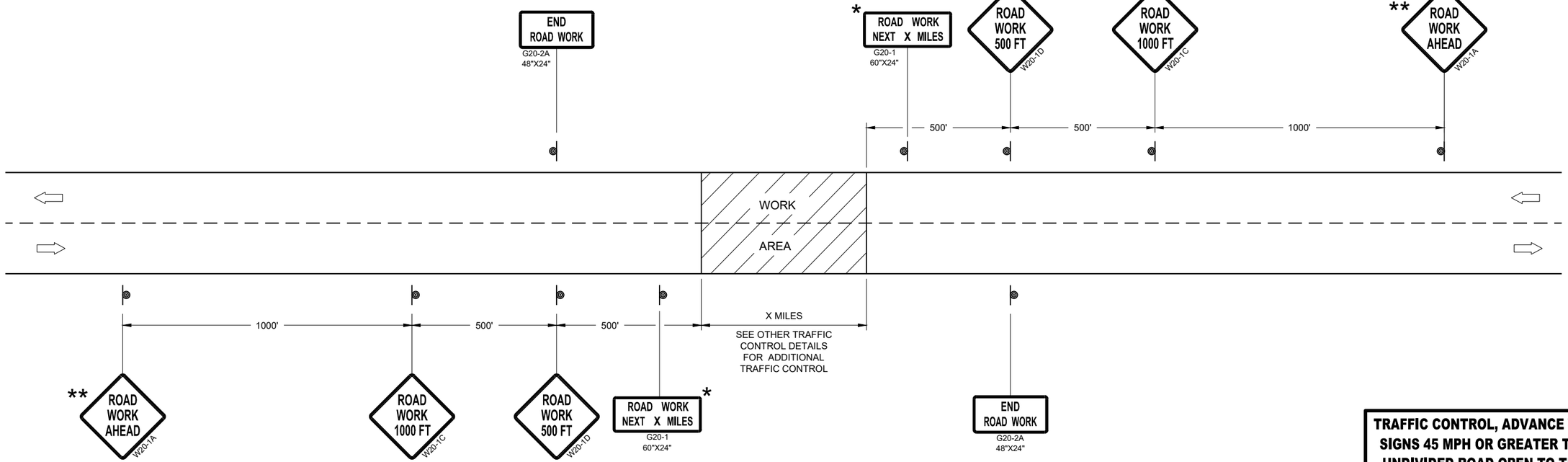
TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C04 - 05

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

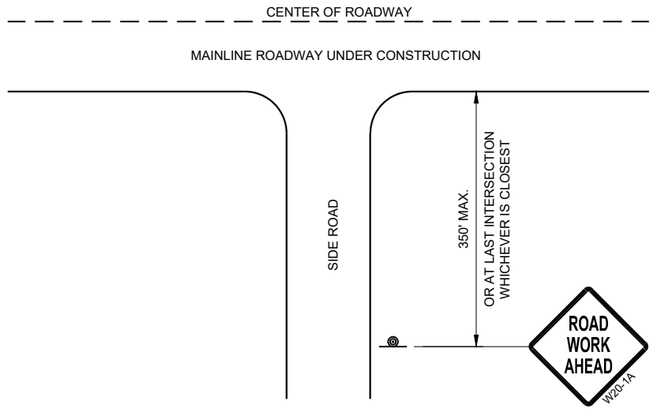
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

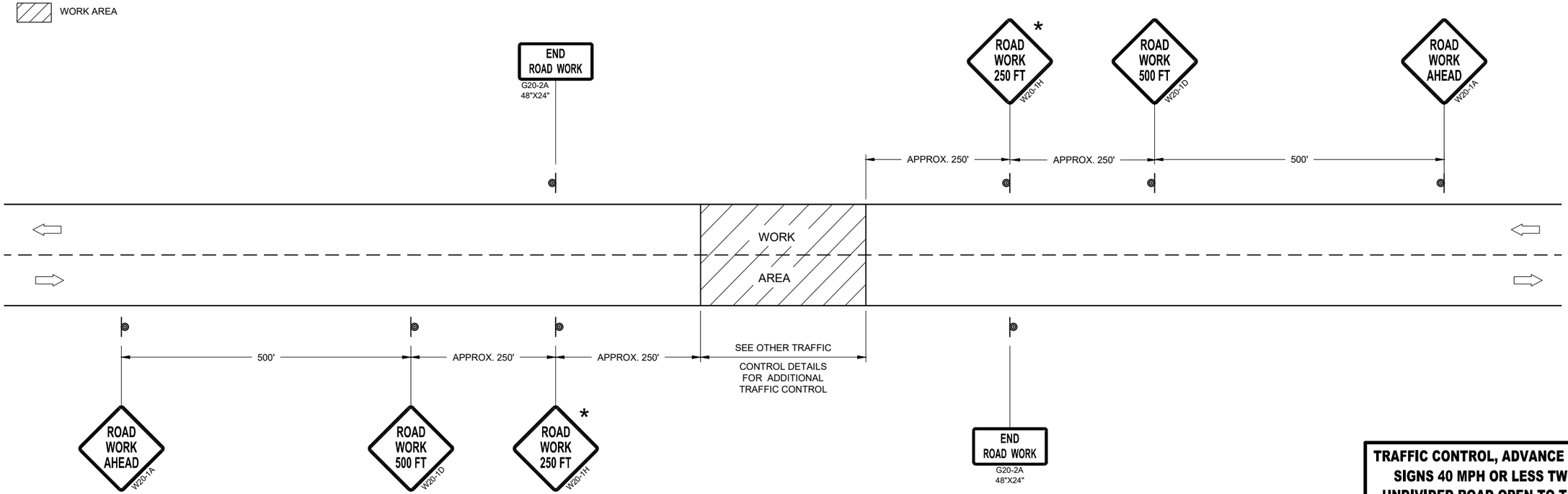
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

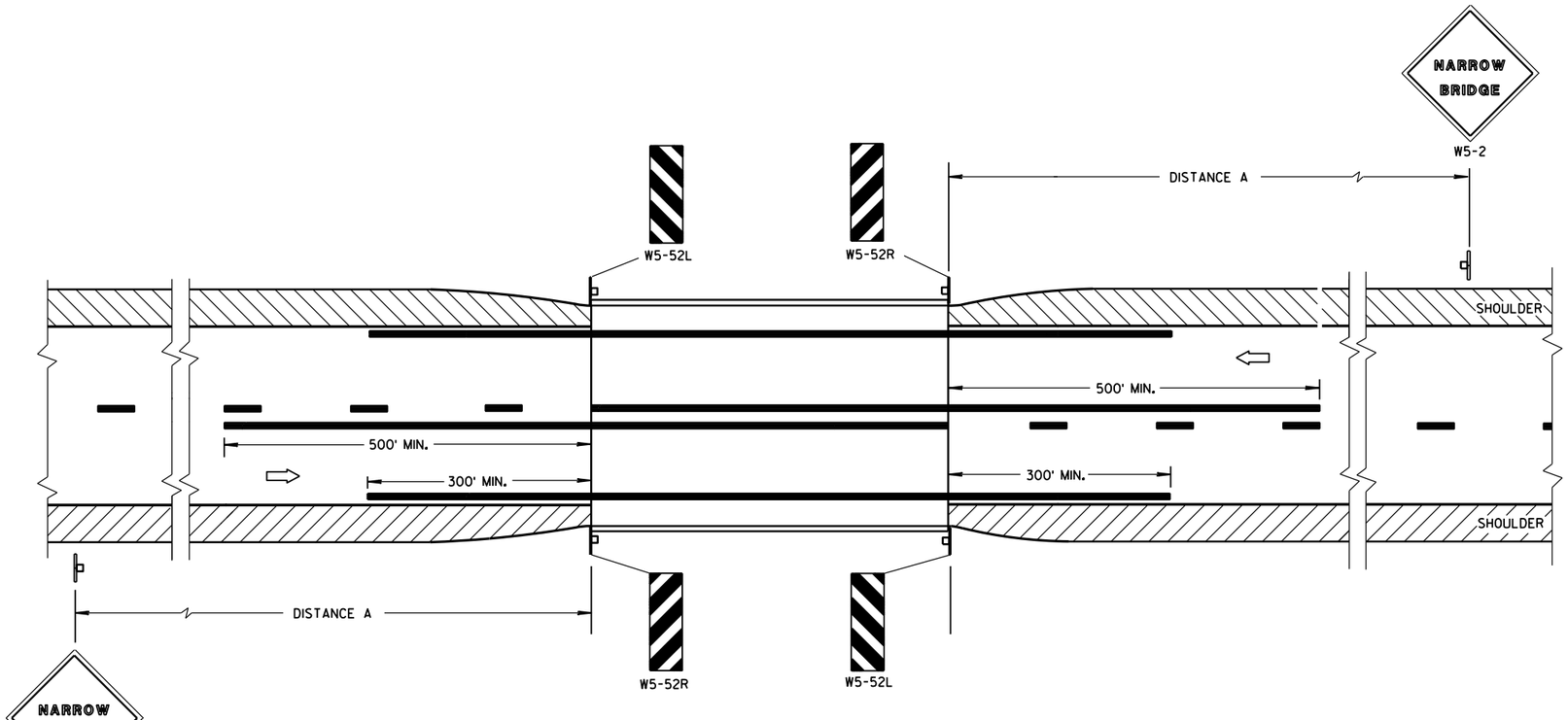
APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C05 - 05



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

GENERAL NOTES

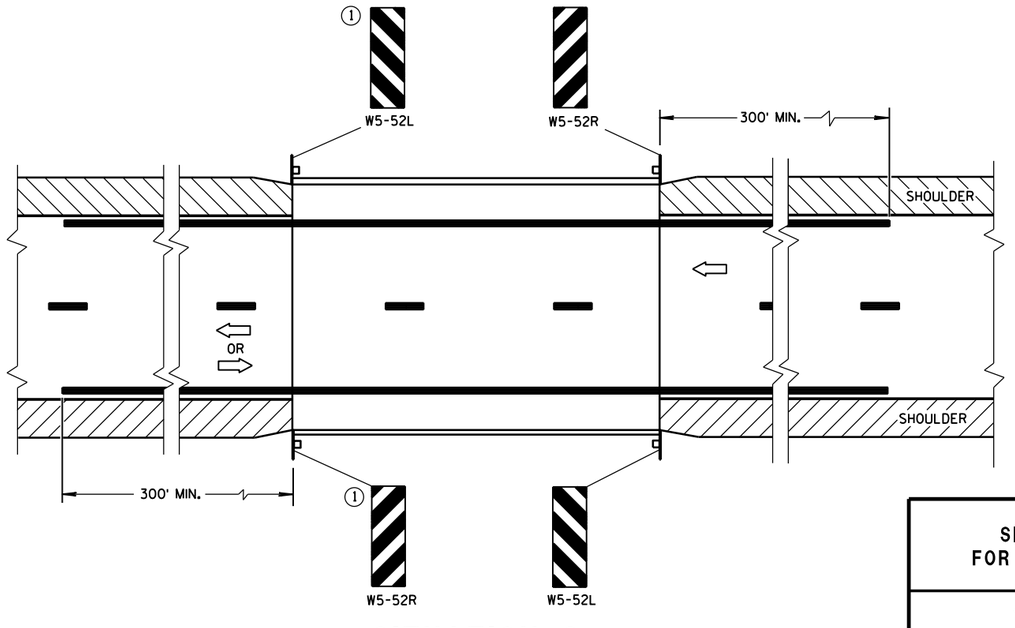
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

(1) OMIT ON ONE-WAY TRAVELLED WAYS.

⇨ DIRECTION OF TRAFFIC



SITUATION 2

WARRANTING CRITERIA:

1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

SIGNING & MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

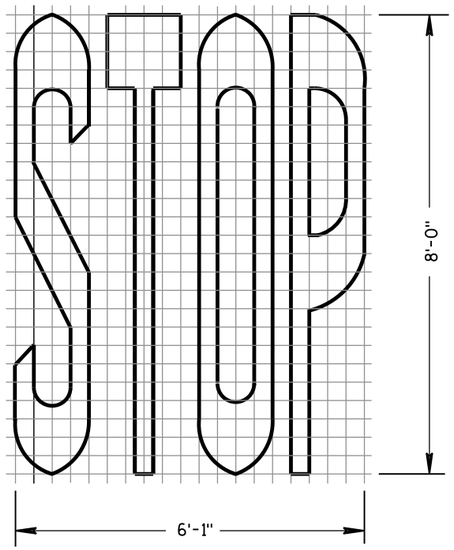
APPROVED
June 2017 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MAR ...IEER
FHWA

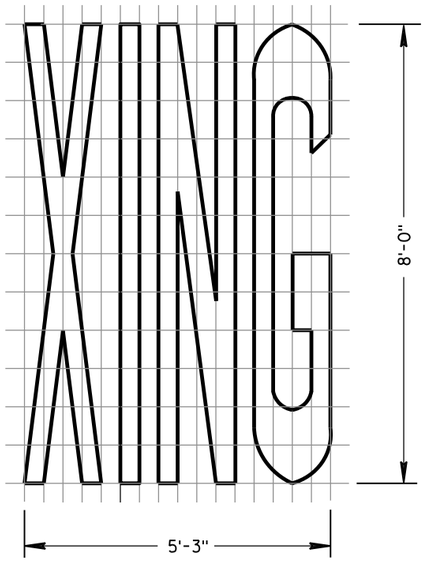
6

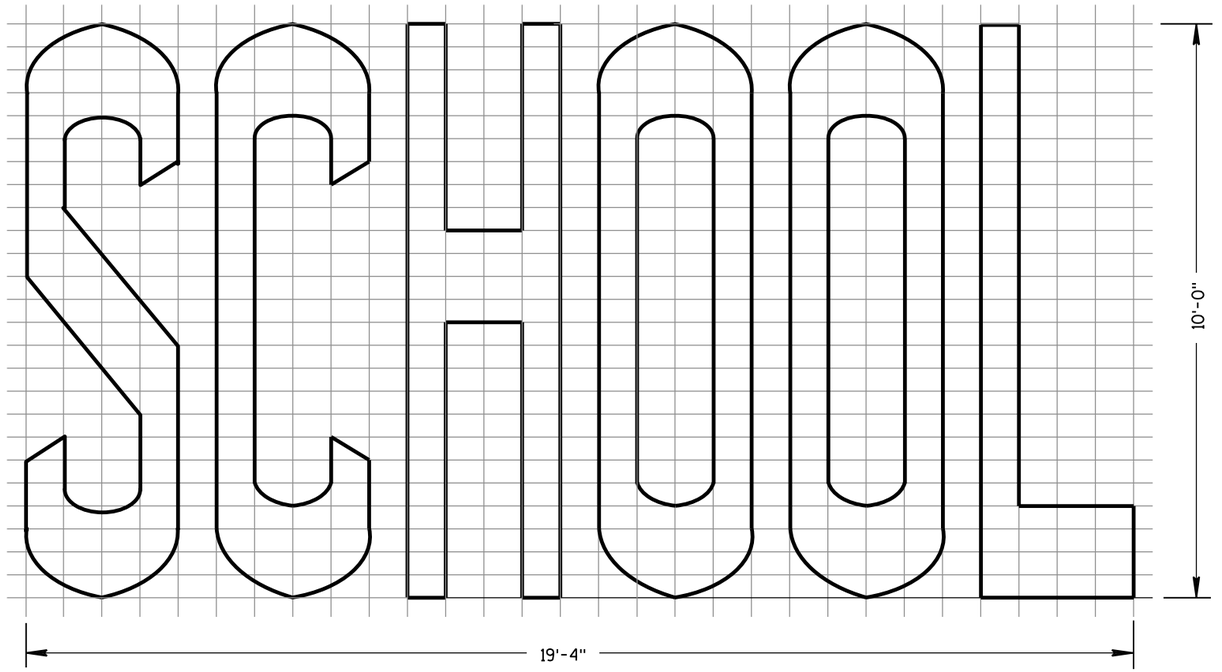
S.D.D. 15 C 6-9

GENERAL NOTES

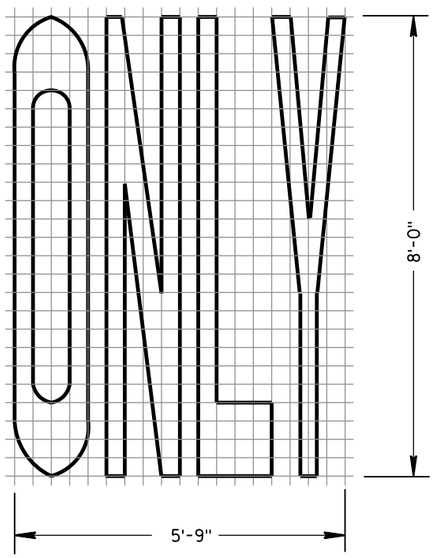
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

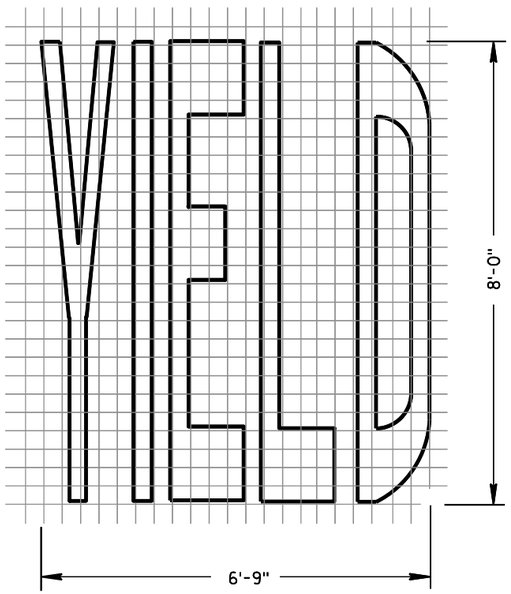


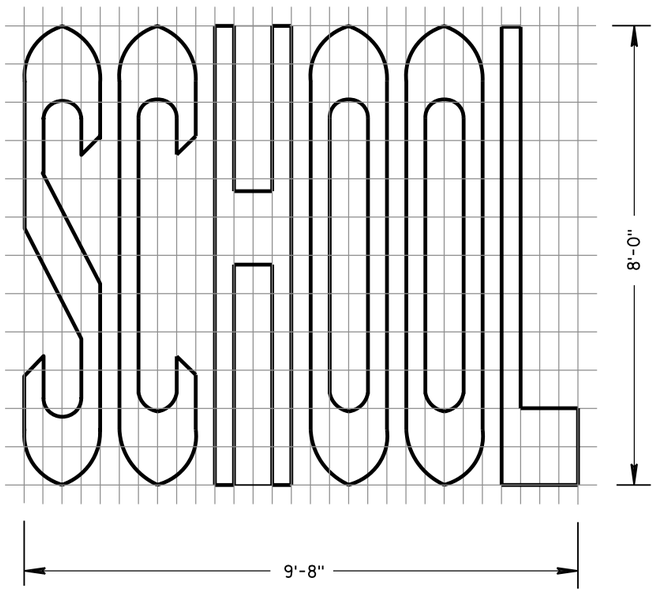




TWO-LANE







SINGLE-LANE

PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MARK
EER
FHWA

6

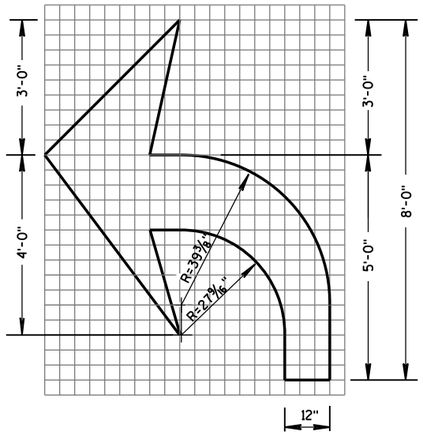
S.D.D. 15 C 7-14b

6

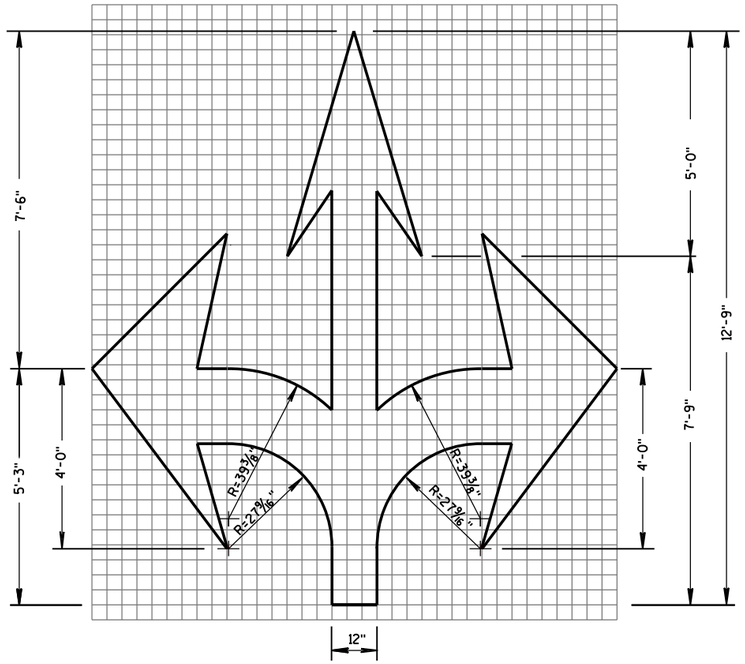
S.D.D. 15 C 7-14b

GENERAL NOTES

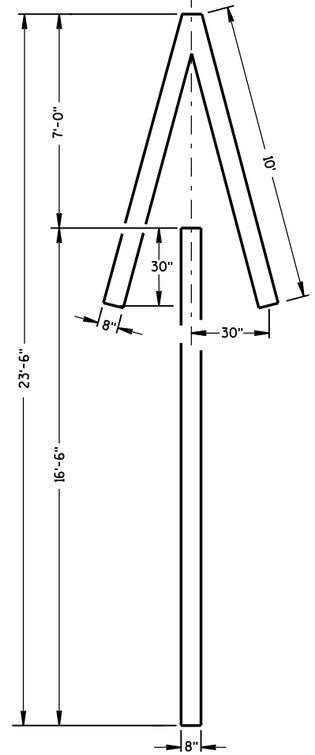
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



TYPE 2



TYPE 6



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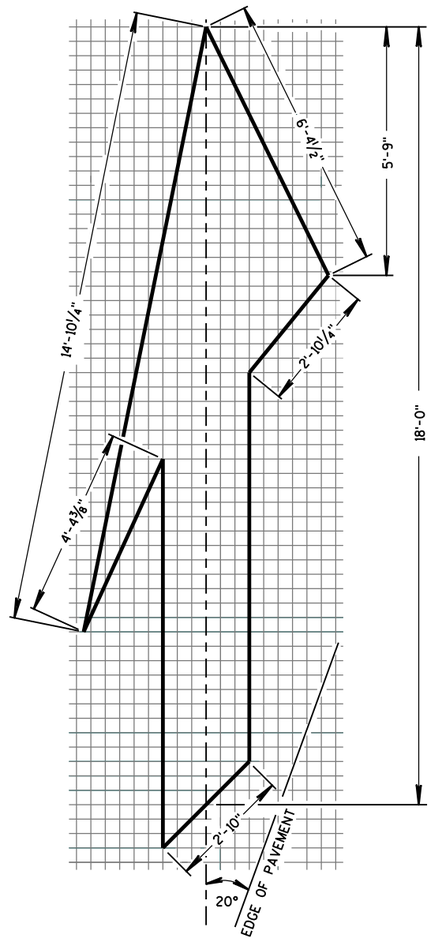
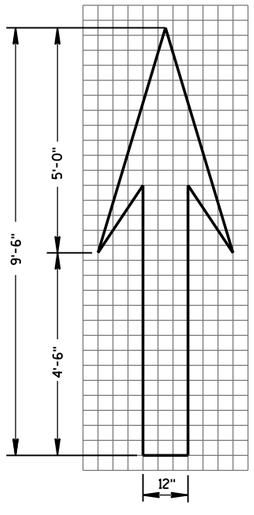
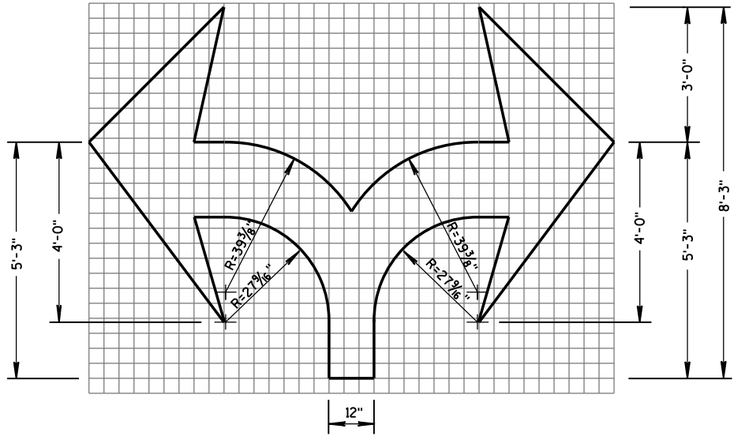


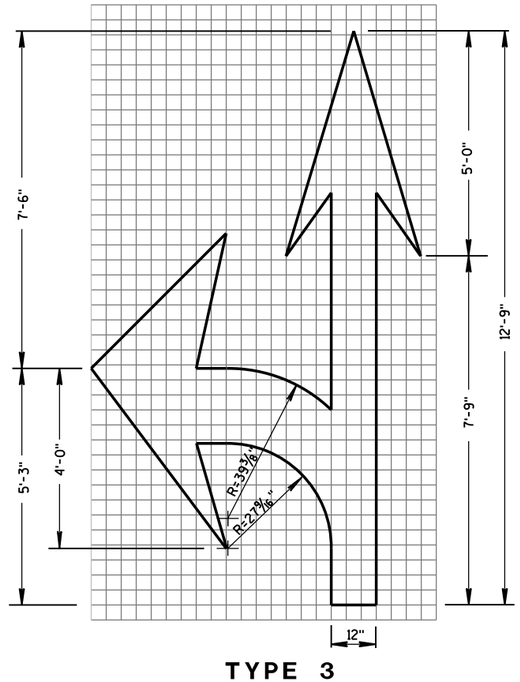
TYPE 5 LANE DROP ARROW



TYPE 1



TYPE 7



TYPE 3

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MAR[illegible]NEER
FHWA

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S.D.D. 15 C 7-14c

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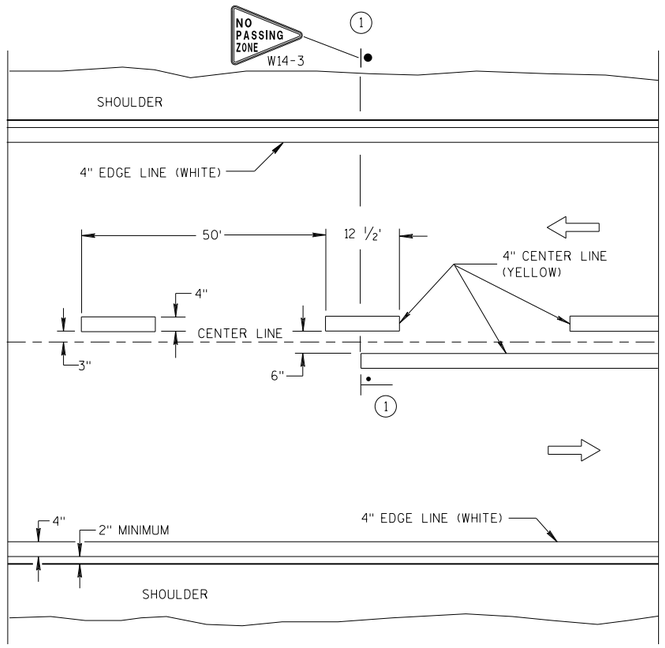
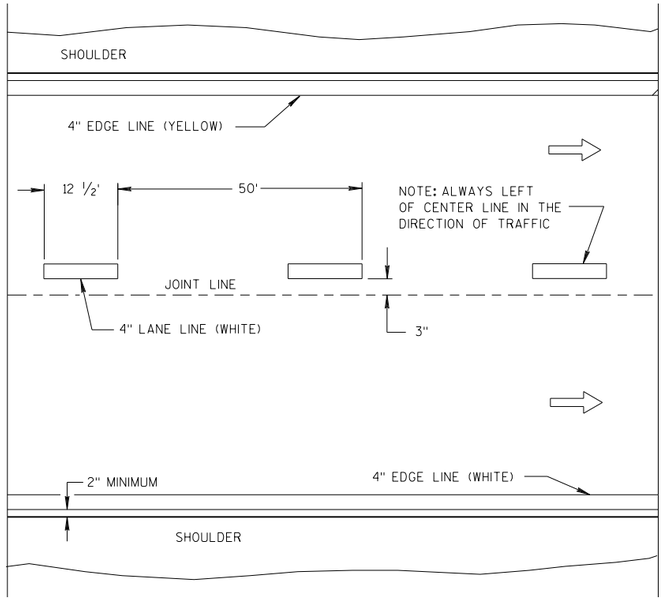
S.D.D. 15 C 7-14c



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

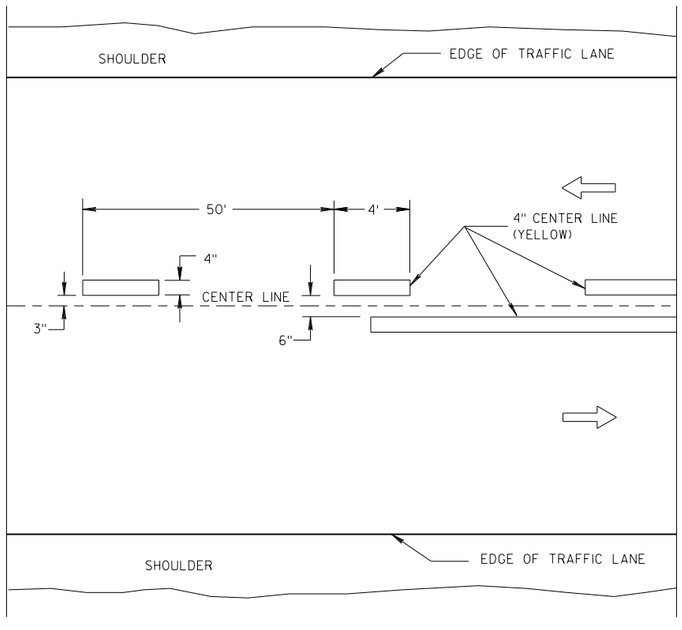
NOTE

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

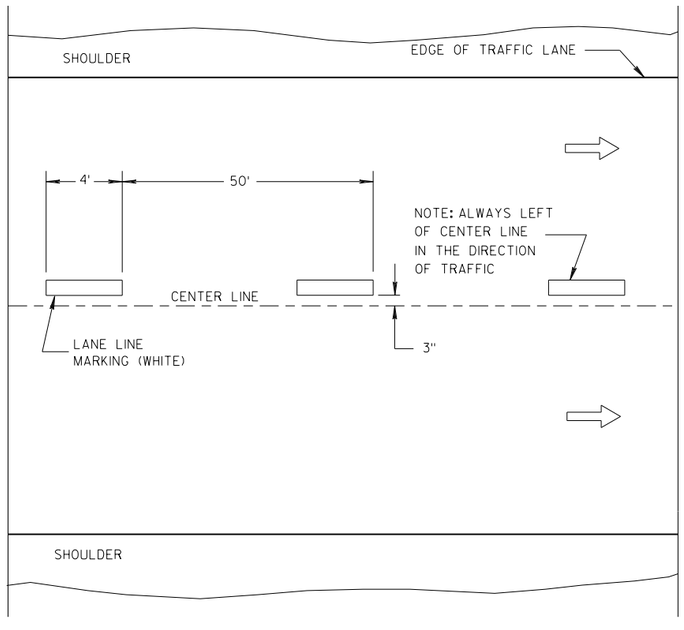
LEGEND

"T" MARKING

POST MOUNTED SIGN



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

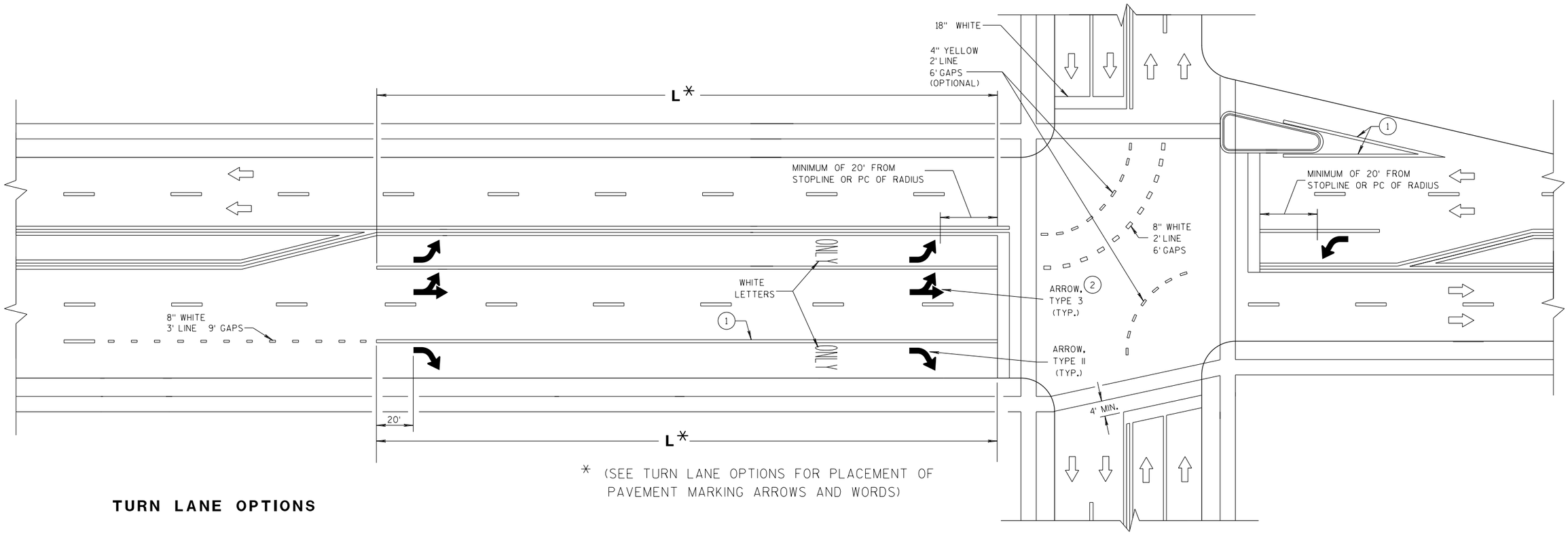
7/2018 — DATE

/S/ Matthew R. Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

6

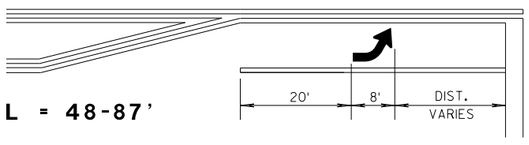
S.D.D. 15 C 8-19a



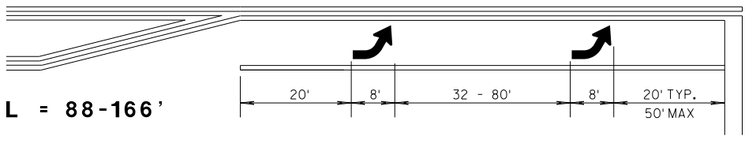
* (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

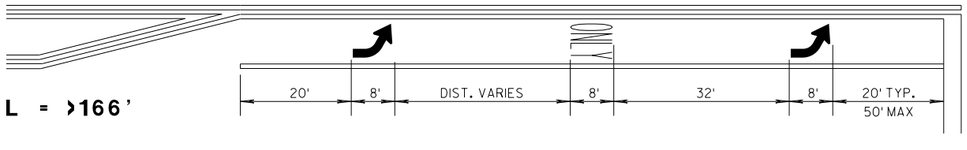
LENGTH OF TURN BAY (**L**) OF 0-47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



L = 48-87'



L = 88-166'



L = >166'

GENERAL NOTES

(1) 8" WHITE

(2) QUANTITY AND LOCATION OF TYPE 3 ARROW ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION, THE ARROWS AND ONLY MARKING ARE ELIMINATED.

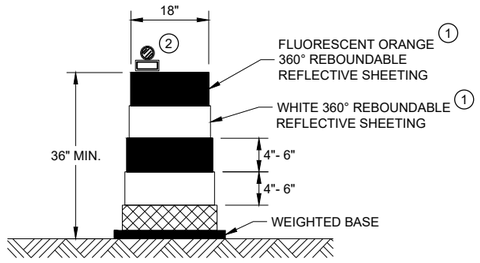
⇨ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

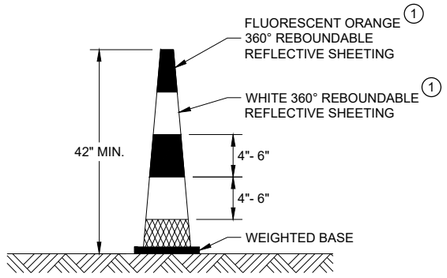
PAVEMENT MARKING (TURN LANES)

GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

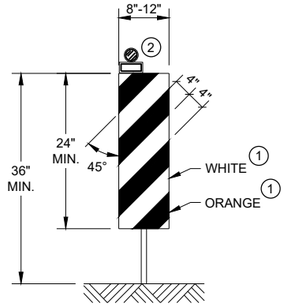


DRUM



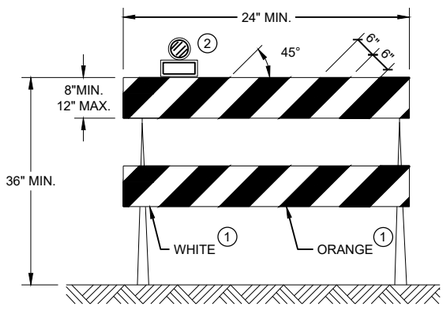
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS



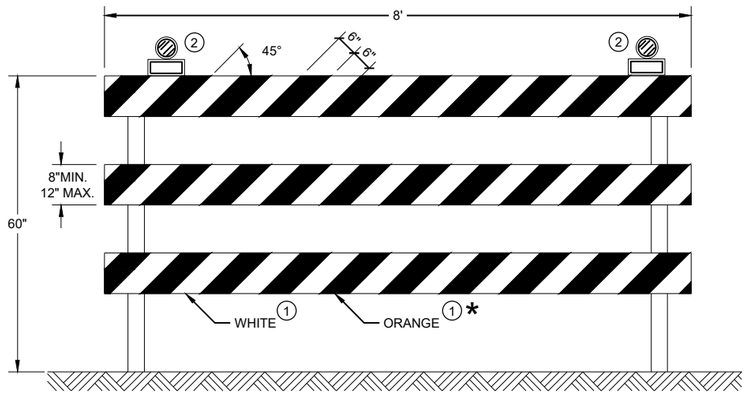
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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LEGEND

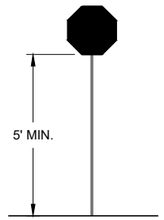
SIGN ON PORTABLE OR PERMANENT SUPPORT

TEMPORARY PORTABLE RUMBLE STRIP ARRAY

DIRECTION OF TRAFFIC

WORK AREA

FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF



STOP/SLOW PADDLE ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

(1) FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

(3) EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

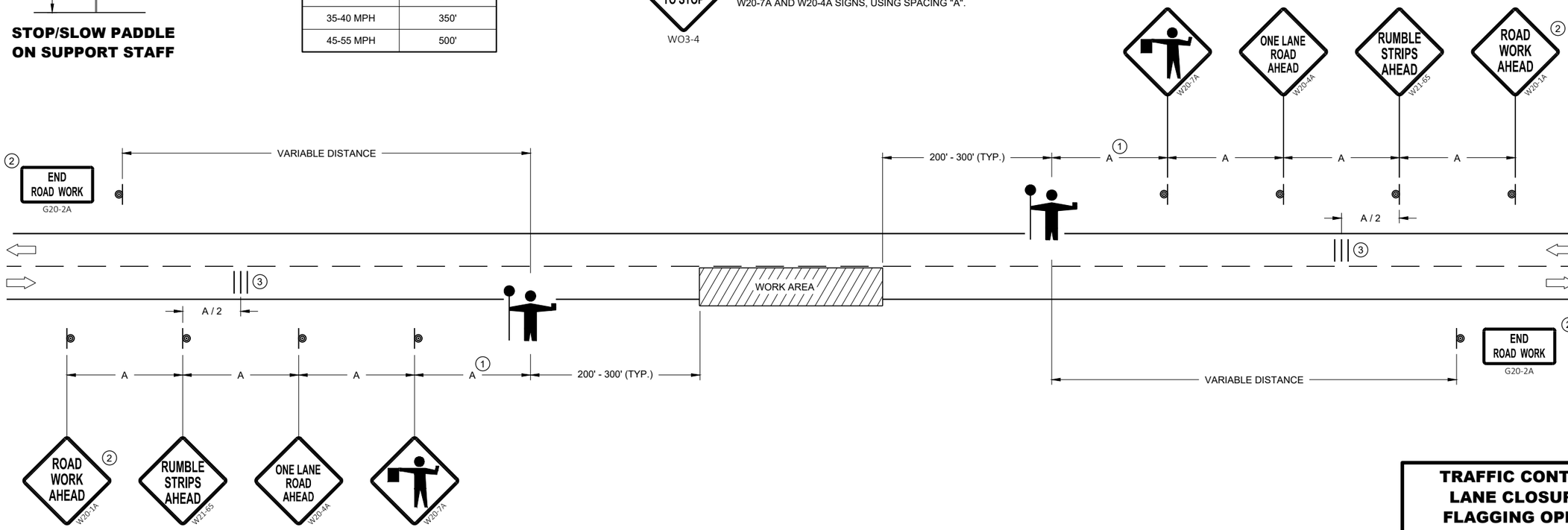
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

WO3-4

USE OF WO3-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

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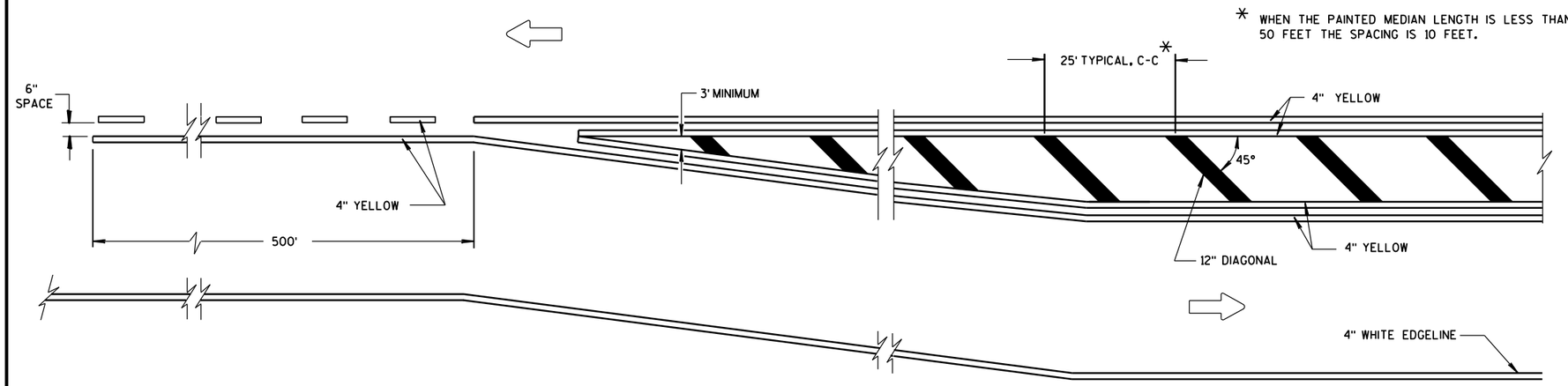
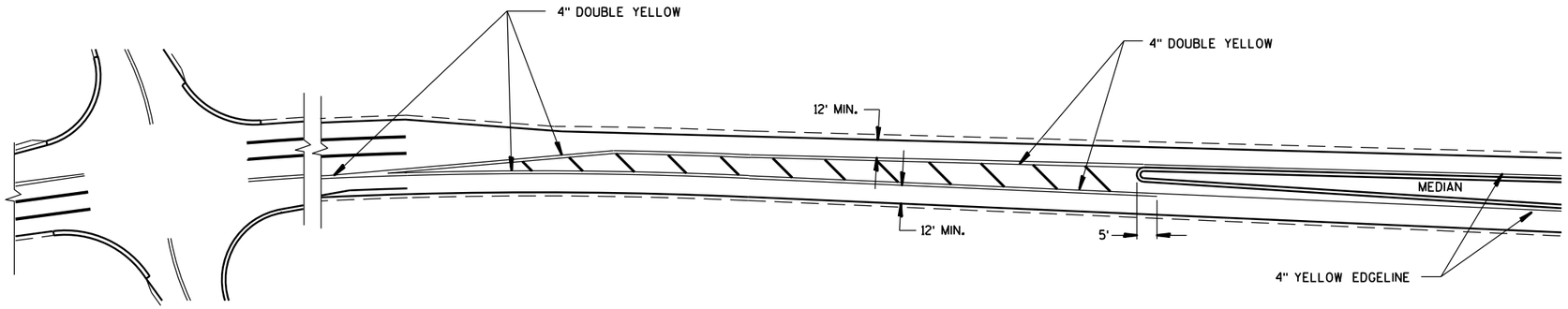


MEDIAN ISLAND DETAIL

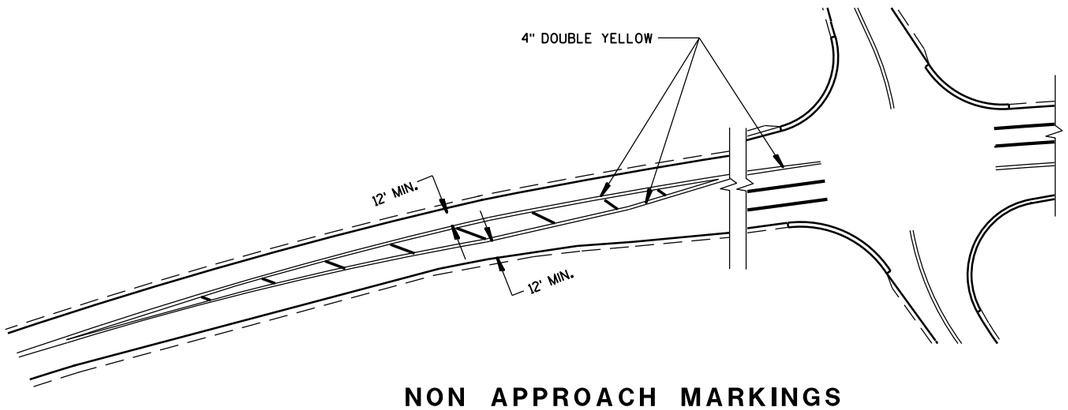
GENERAL NOTE

DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT WIDEST POINT.

DIRECTION OF TRAVEL



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON APPROACH MARKINGS

MEDIAN ISLAND MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE FHWA	/S/ Matthew Rauch STATE SIGNING AND MARK EER

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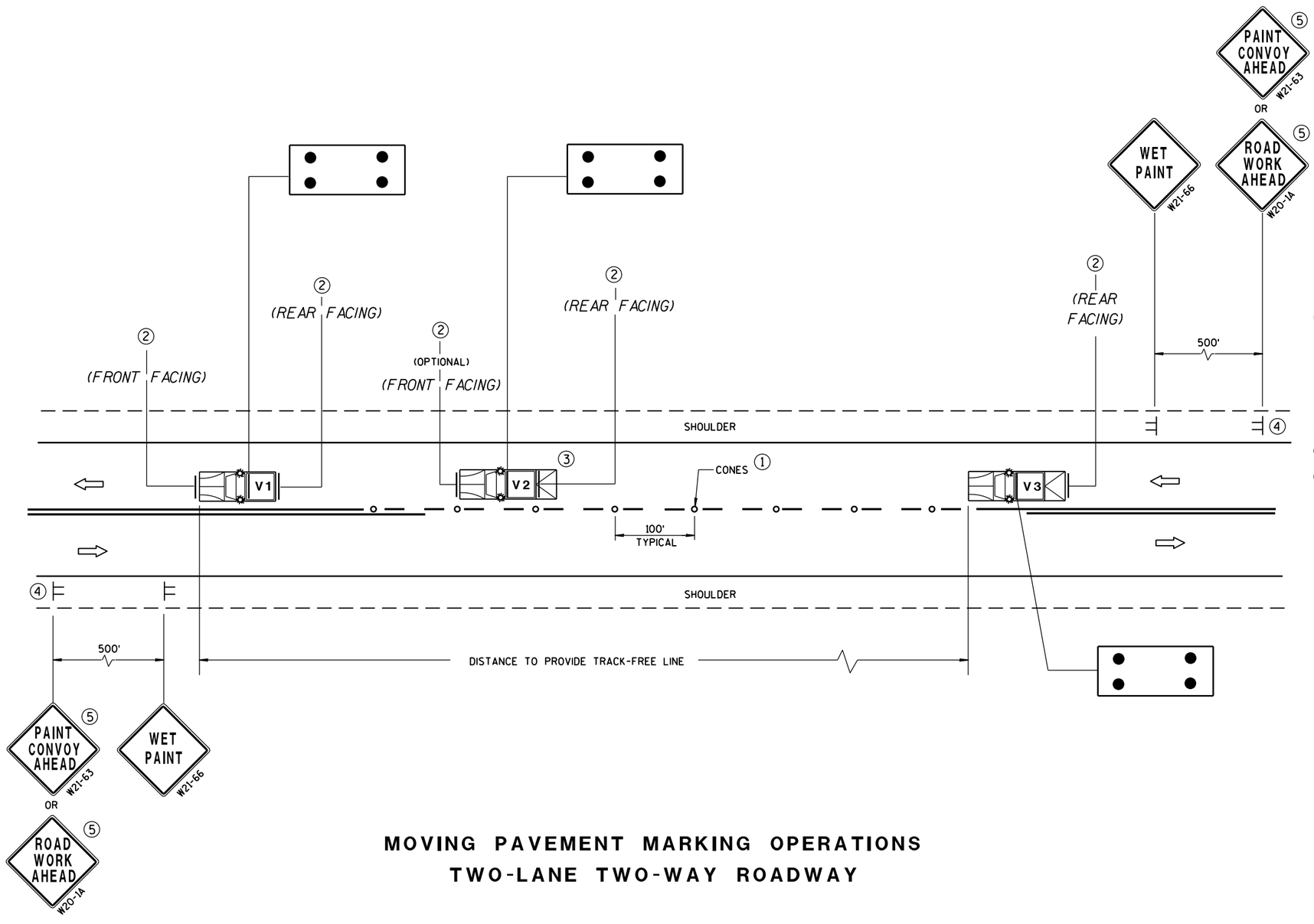

MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

(1) CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

(2) USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.

WET PAINT W21-64 OR WET PAINT W21-64

(3) OPTIONAL TRUCK-MOUNTED ATTENUATOR.

(4) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

(5) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

- **V1** LEAD VEHICLE
- **V2** SHADOW VEHICLE
- **V3** TRAIL VEHICLE WITH TMA
- **TMA** TRUCK-MOUNTED ATTENUATOR
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- CONES
- FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
Sept., 2017 DATE	/S/ Andrew Heldtke WORK ZONE ENGR
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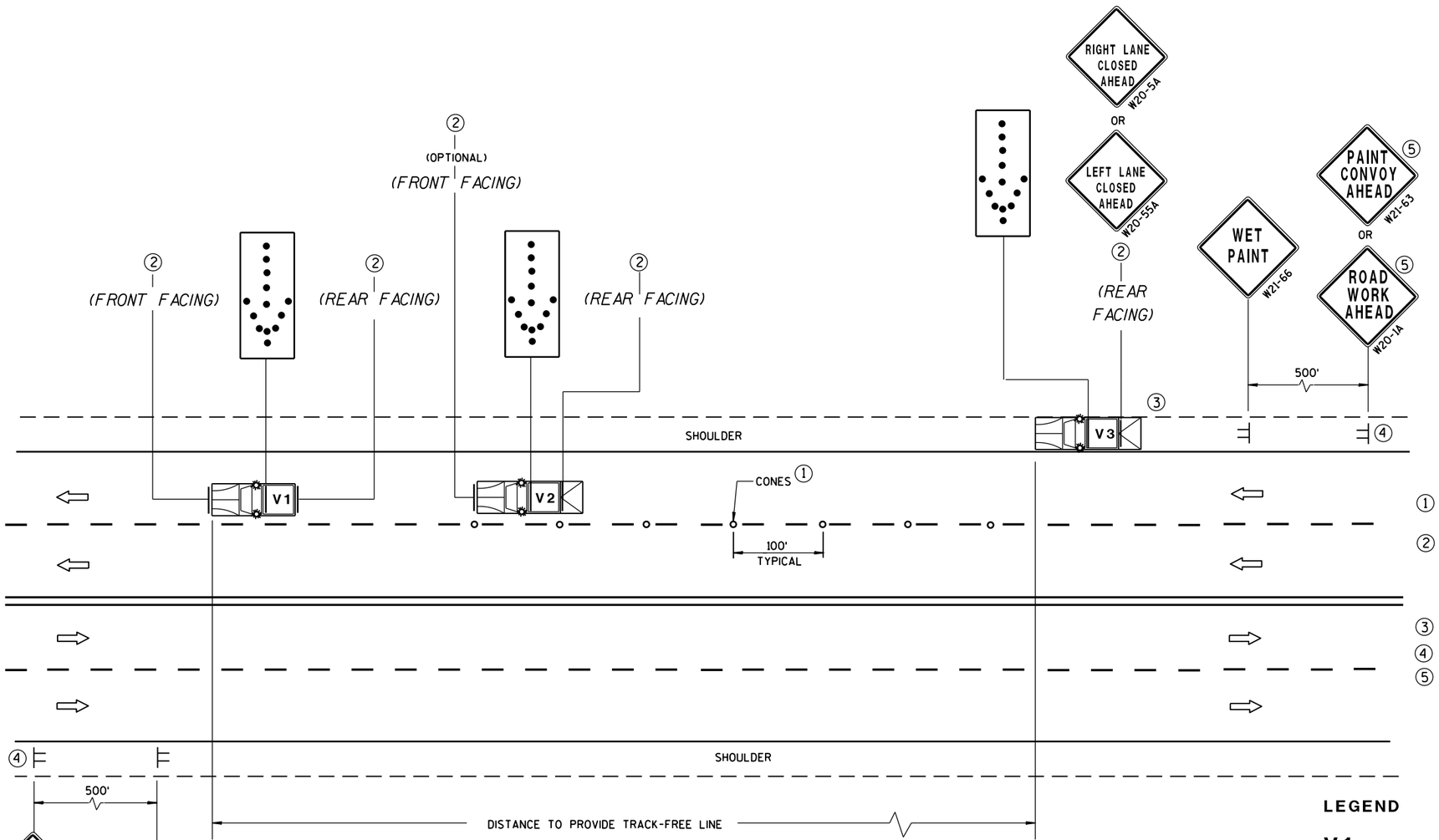
S.D.D. 15 C 19-5a



MOVING PAVEMENT MARKING OPERATIONS
MULTI-LANE UNDIVIDED ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

FOR EDGELINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE OR LANELINE MARKING FOR MULTILANE UNDIVIDED ROADWAYS.

(1) CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

(2) USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.

WET PAINT W21-64 OR WET PAINT W21-64

(3) OPTIONAL TRUCK-MOUNTED ATTENUATOR.

(4) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

(5) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

V1	LEAD VEHICLE
V2	SHADOW VEHICLE
V3	TRAIL VEHICLE WITH TMA
TMA	TRUCK-MOUNTED ATTENUATOR
	SIGN ON TEMPORARY SUPPORT
	DIRECTION OF TRAFFIC
	CONES
	FLASHING ARROW PANEL (MERGE)

MOVING PAVEMENT MARKING OPERATION
MULTI-LANE UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2017 — DATE
/S/ Andrew Heldtke — WORK ZONE ENGIN
FHWA

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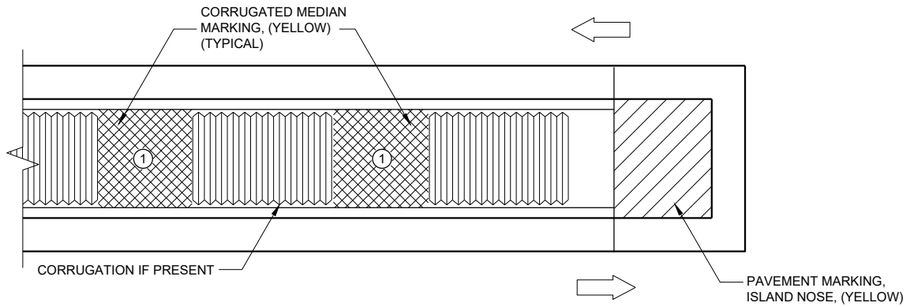
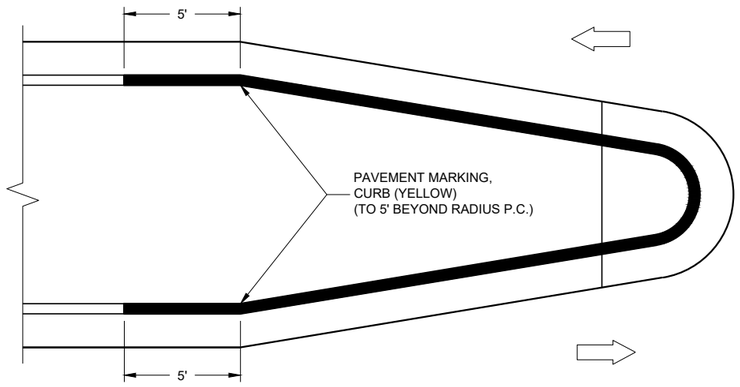
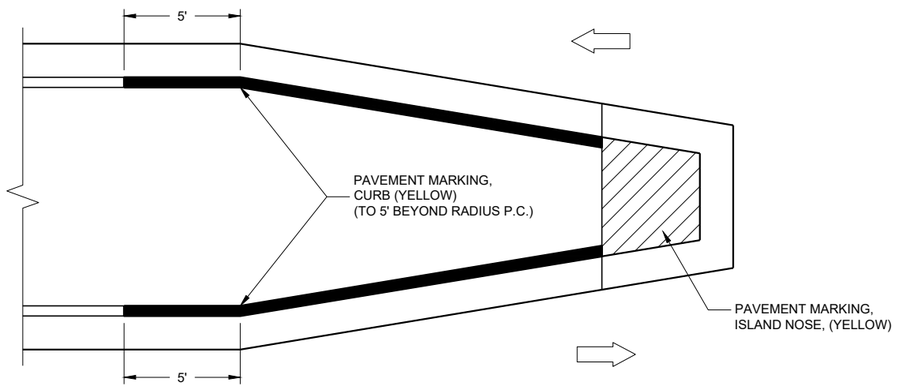


MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE

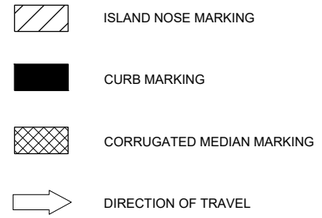


MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF
PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.



PAVEMENT MARKINGS
(ISLANDS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

7/2018
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MAF ENGINEER

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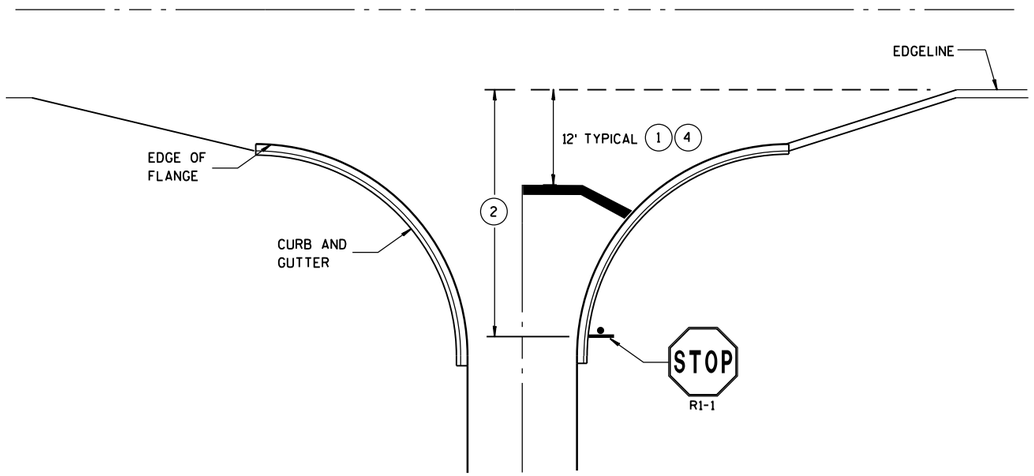
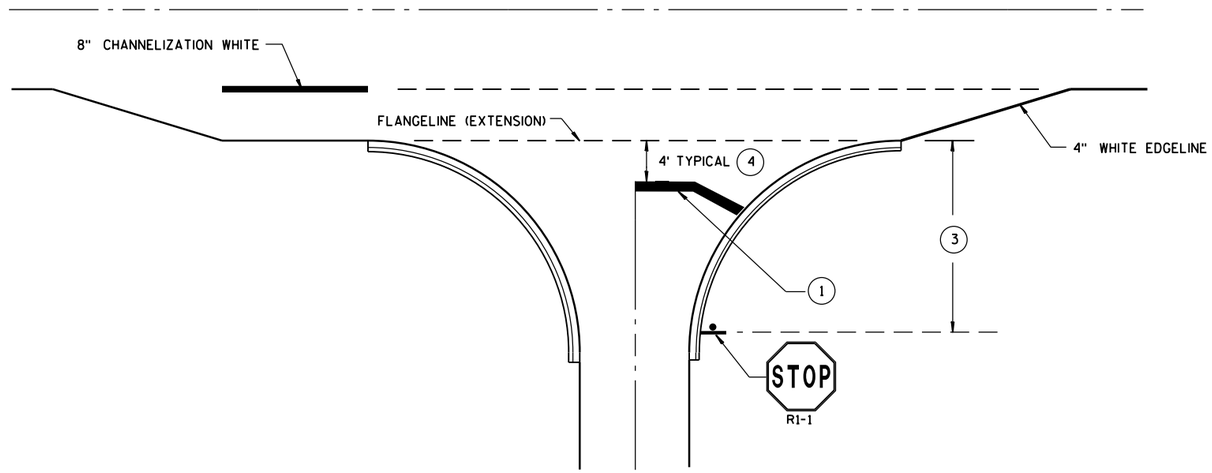
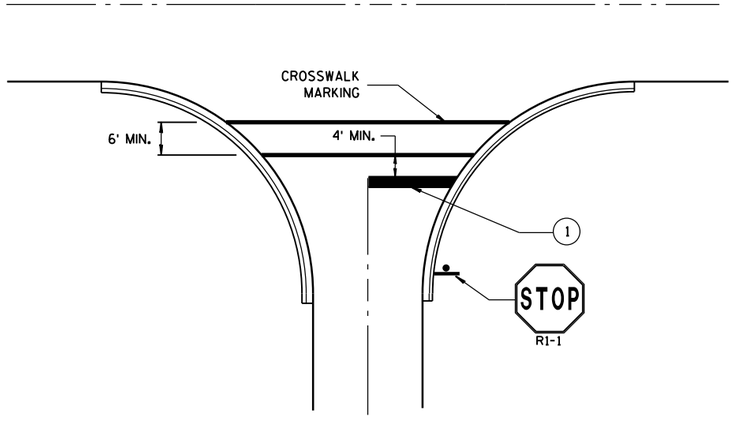


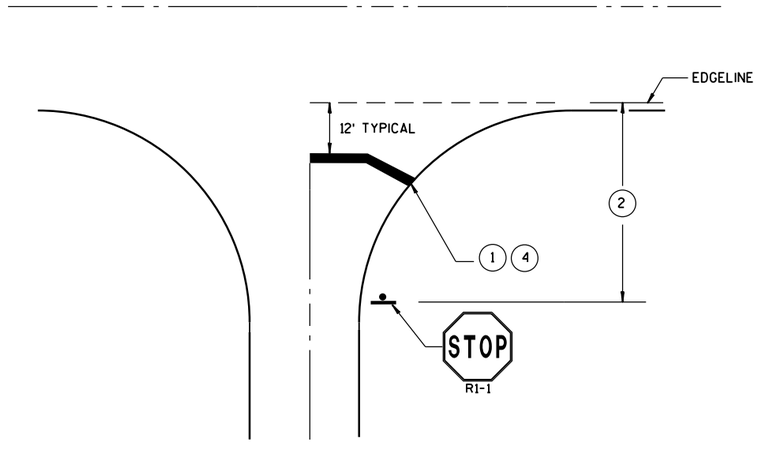
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

(1) 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.

(2) IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.

(3) IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.

(4) MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

Sept., 2017 — DATE

/S/ Matthew R. Rauch — STATE SIGNING AND MAR ... NEER

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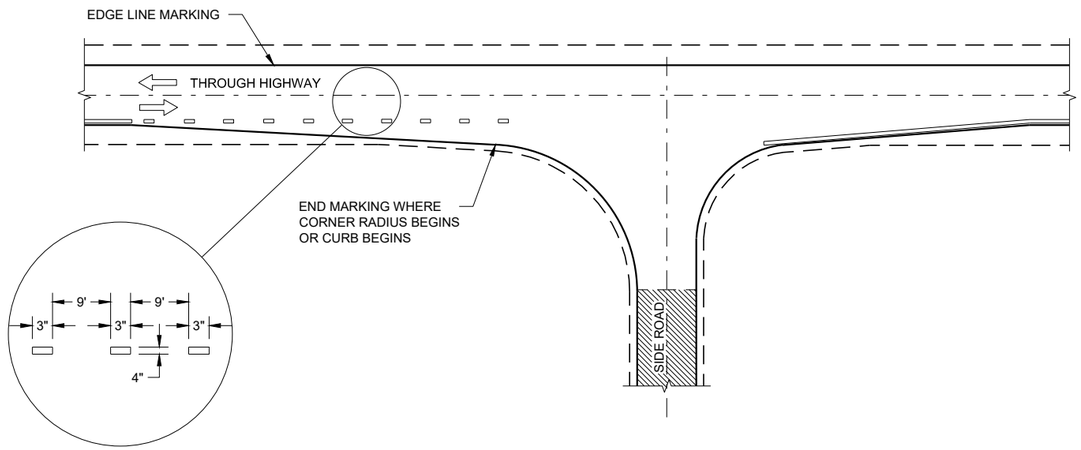
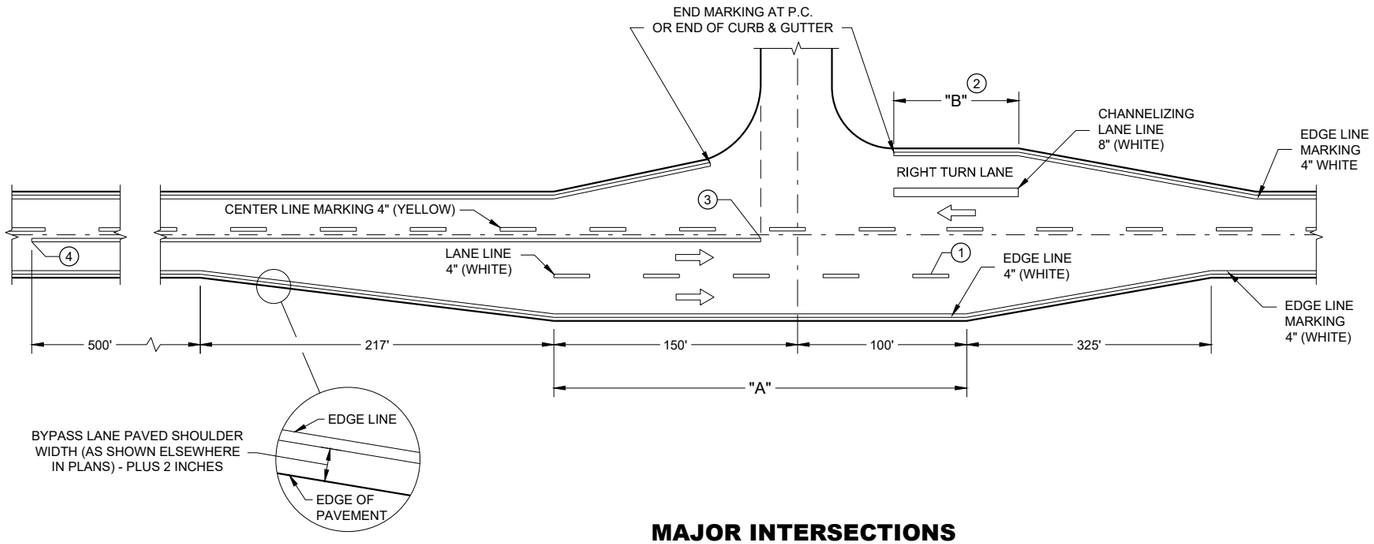


MINOR INTERSECTION



MAJOR INTERSECTIONS

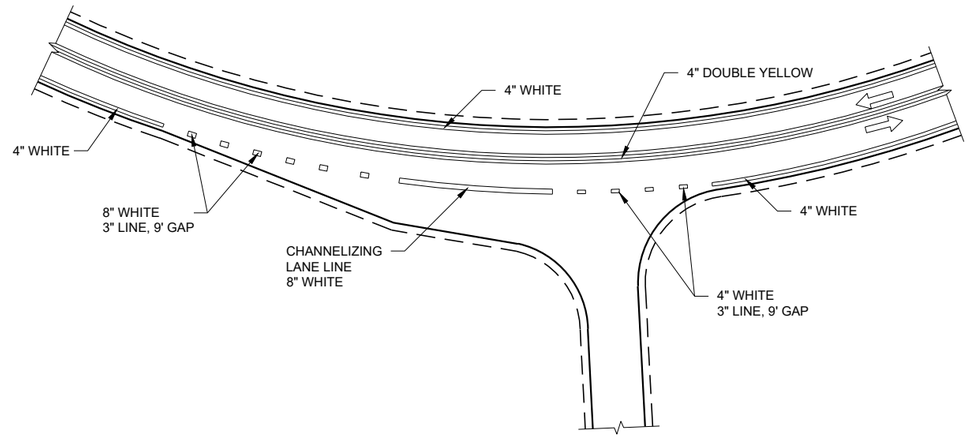
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

1. WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
2. WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
3. BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
4. BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE

PAVEMENT MARKING (INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 15C35 - 03a

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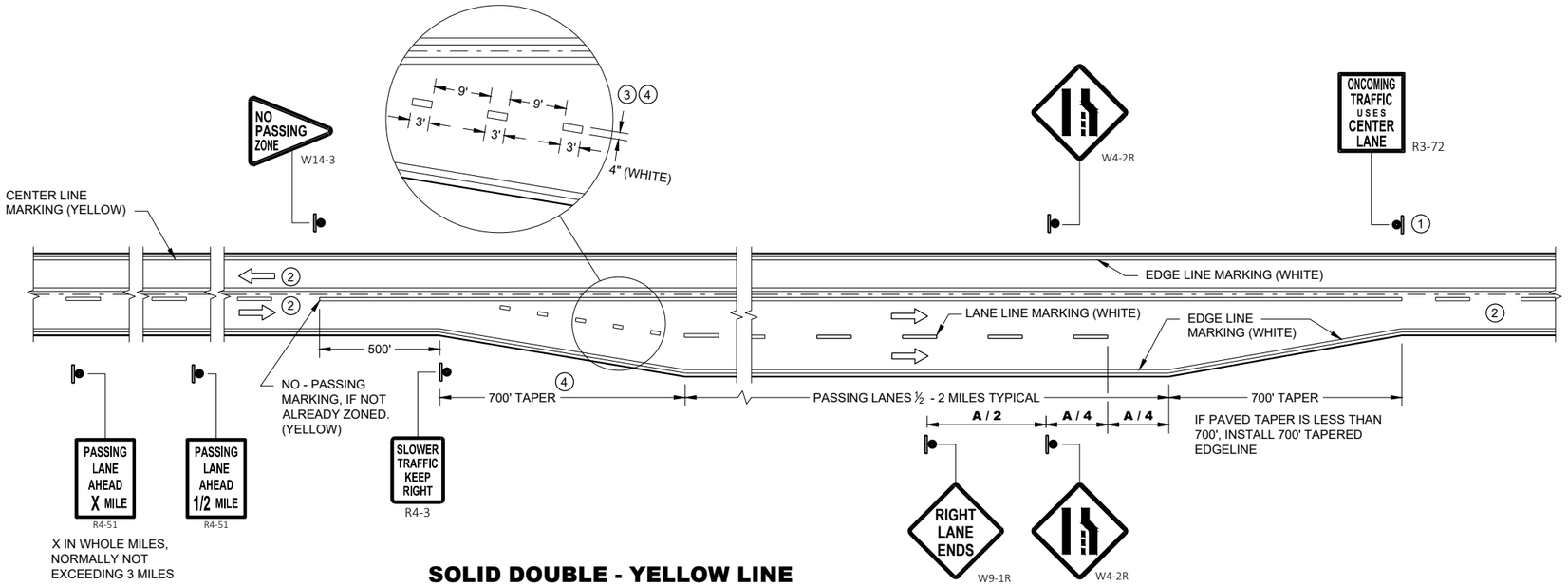
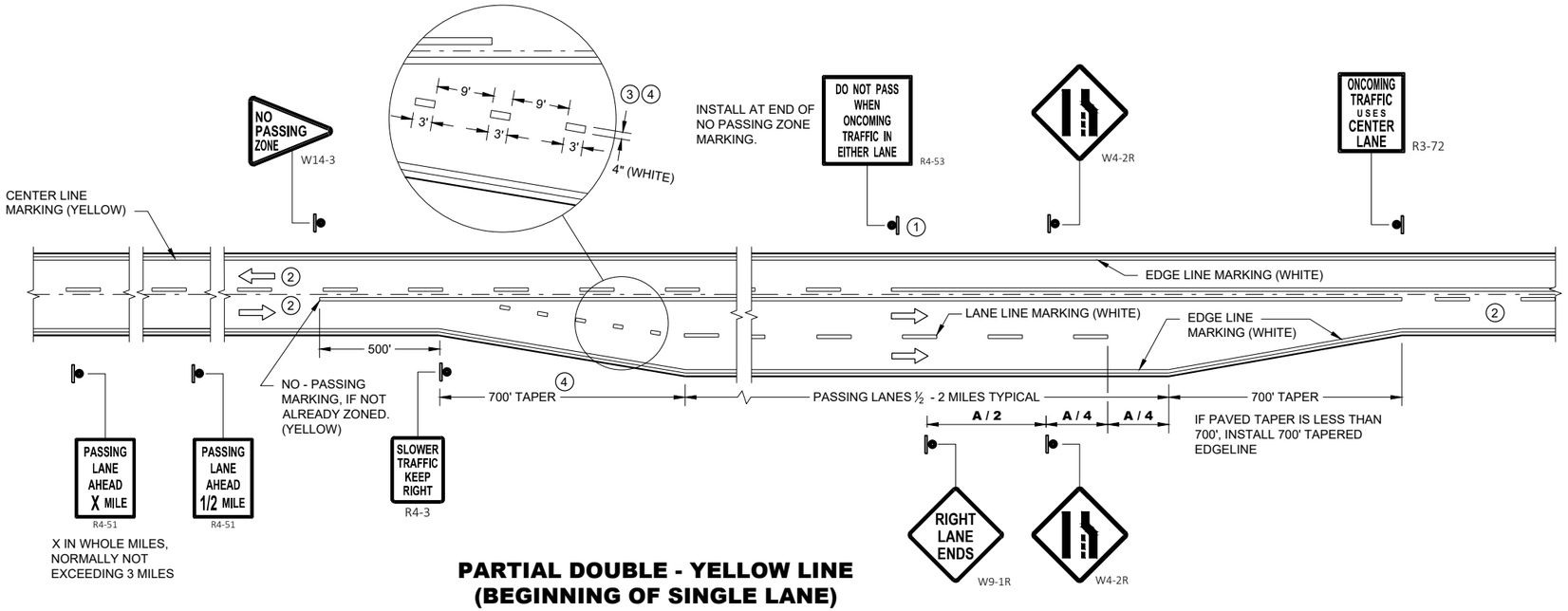
SDD 15C35 - 03a



SOLID DOUBLE - YELLOW LINE
(THROUGHOUT ENTIRE PASSING / CLIMBING LANE)



PARTIAL DOUBLE - YELLOW LINE
(BEGINNING OF SINGLE LANE)

GENERAL NOTES

1. SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
2. THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
3. THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE, 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
4. WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING / CLIMBLING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

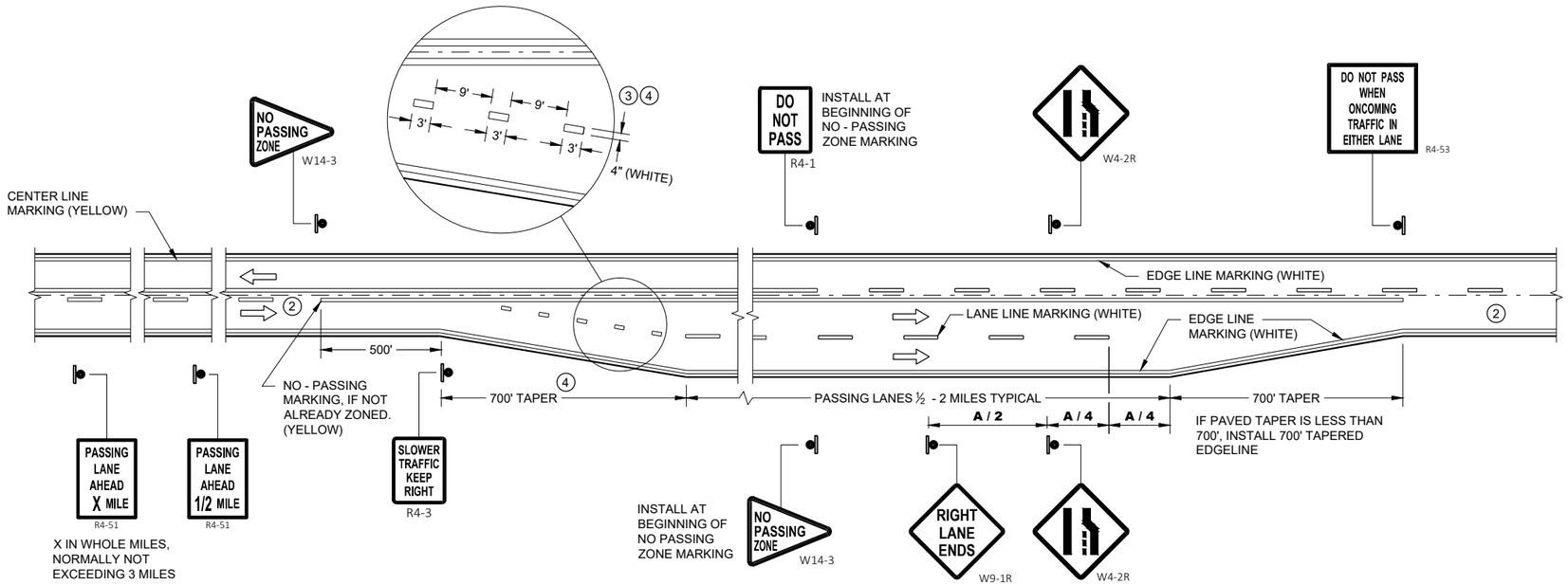
POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	775
50	850
55	950

PAVEMENT MARKING & SIGNING (CLIMBING LANE & PASSING LANE)

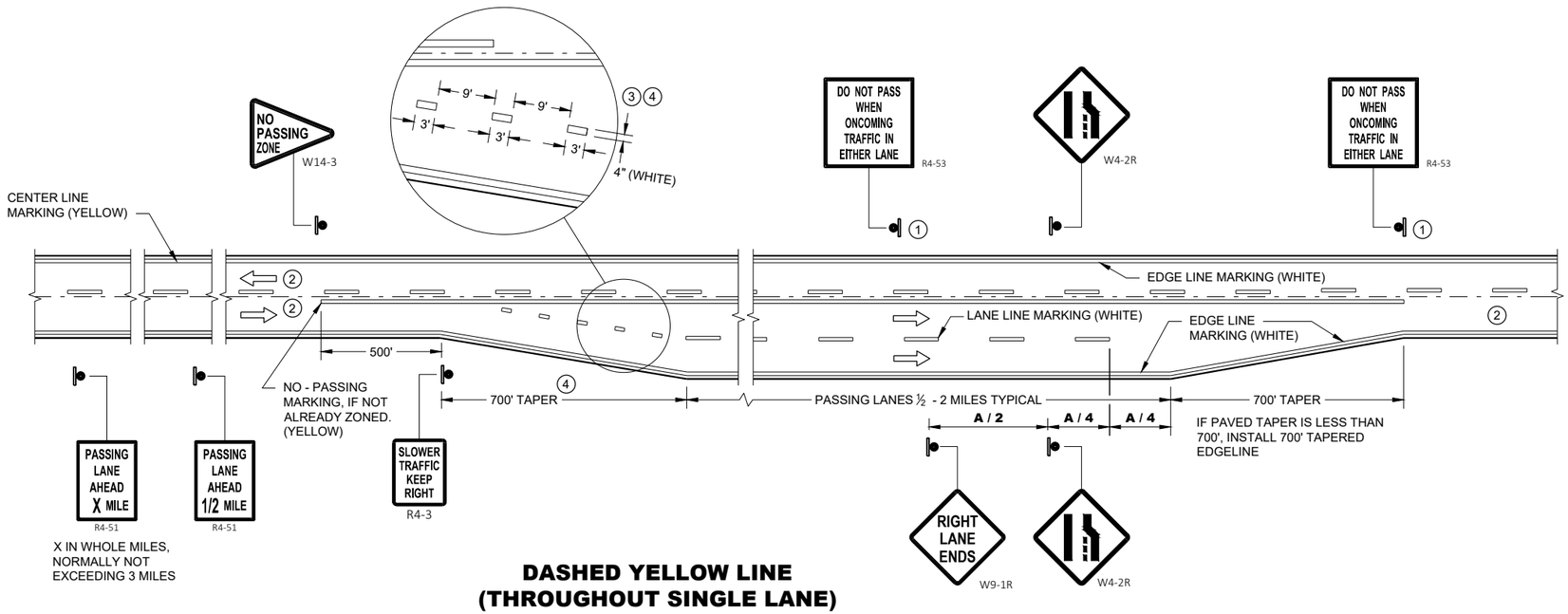
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

SDD 15C35 - 03b

SDD 15C35 - 03b



SOLID DOUBLE - YELLOW LINE
(END OF SINGLE LANE)



DASHED YELLOW LINE
(THROUGHOUT SINGLE LANE)

GENERAL NOTES

1. SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
2. THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
3. THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE, 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
4. WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING / CLIMBLING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	775
50	850
55	950

PAVEMNET MARKING & SIGNING (CLIMBING LANE & PASSING LANE)

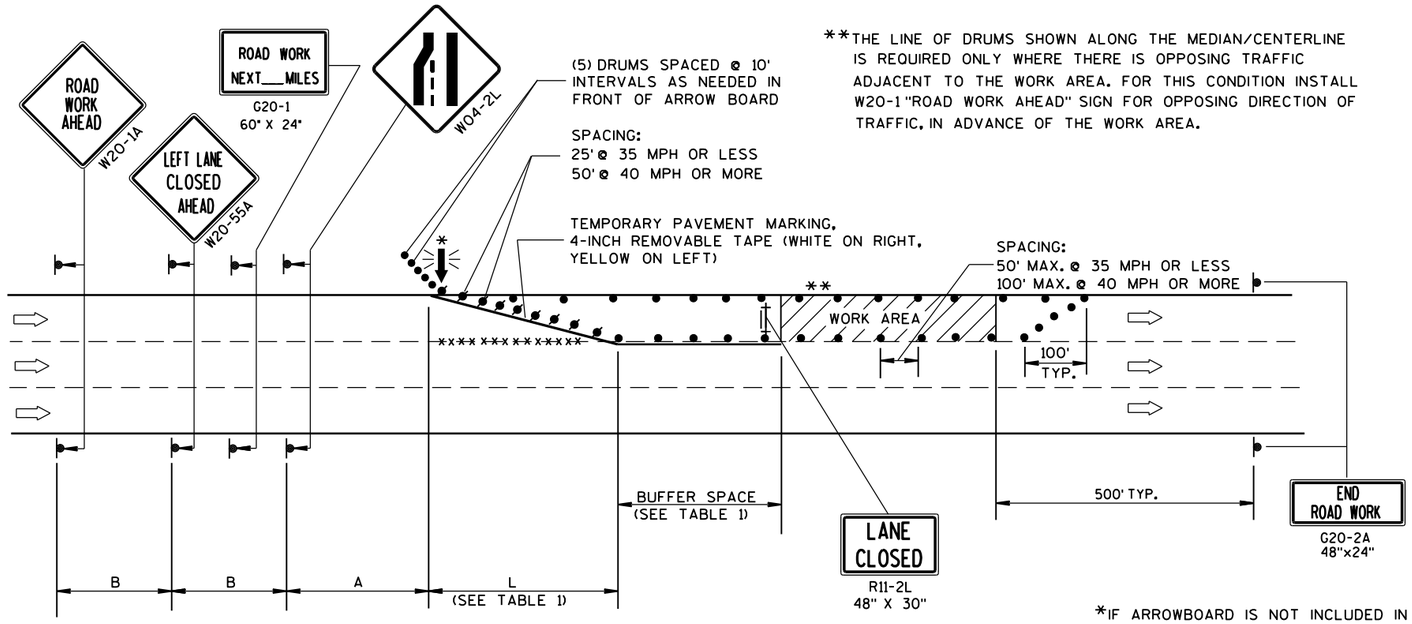
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 DATE
/S/ Matthew R. Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

SDD 15C35 - 03c

SDD15C35 - 03c



**THE LINE OF DRUMS SHOWN ALONG THE MEDIAN/CENTERLINE IS REQUIRED ONLY WHERE THERE IS OPPOSING TRAFFIC ADJACENT TO THE WORK AREA. FOR THIS CONDITION INSTALL W20-1 "ROAD WORK AHEAD" SIGN FOR OPPOSING DIRECTION OF TRAFFIC, IN ADVANCE OF THE WORK AREA.

B=400' AT 25-30 MPH
700' AT 35-40 MPH
1000' AT 45-55 MPH

A=200' AT 25-30 MPH
350' AT 35-40 MPH
500' AT 45-55 MPH

(PLACE BARRICADE AND SIGN APPROX. EVERY 1000' ACROSS THE CLOSED LANE)

*IF ARROWBOARD IS NOT INCLUDED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE A TYPE III BARRICADE WITH W01-6 SIGN IN THE LANE CLOSURE TAPER.

W01-6
48"x24"

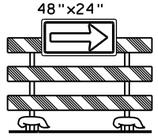


TABLE 1
TAPER AND BUFFER SPACE FOR 12' LANE WIDTH

S	L	BUFFER SPACE
25	125'	55'
30	180'	85'
35	245'	120'
40	320'	170'
45	540'	220'
50	600'	280'
55	660'	335'

FOR LANE WIDTH OTHER THAN 12':

L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

L = TAPER LENGTH IN FEET

S = NON-CONSTRUCTION SPEED LIMIT (MPH)

W = WIDTH OF LANE CLOSURE

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016 — DATE
FHWA

/S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE SAFETY ENGINEER — IC

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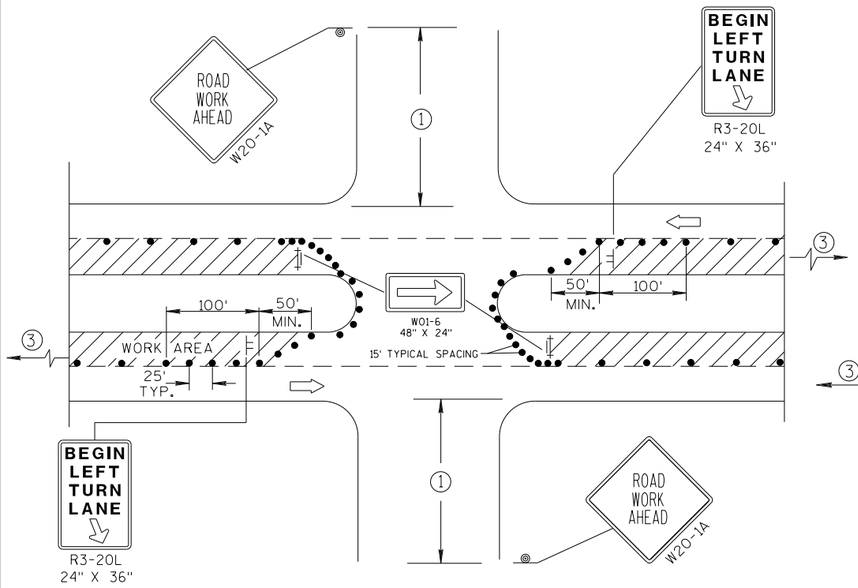
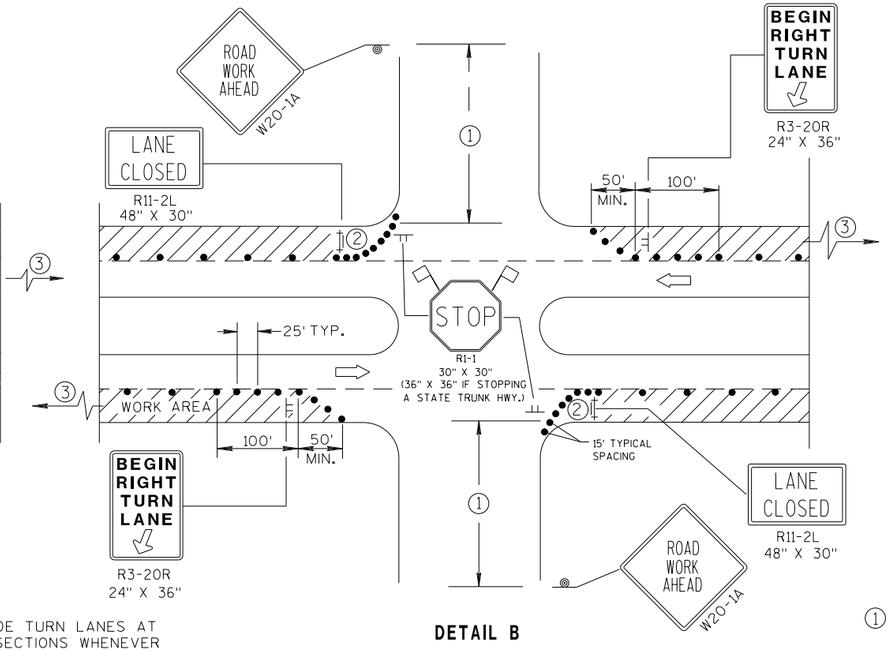
S.D.D. 15 D 20-4



DETAIL A

FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING

PROVIDE TURN LANES AT INTERSECTIONS WHENEVER STAGING OF WORK ALLOWS. TAPER AND TURN LANE LENGTHS BASED ON FIELD CONDITIONS AS APPROVED BY THE ENGINEER.



DETAIL B

FOR RIGHT LANE CLOSURE AT INTERSECTION

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

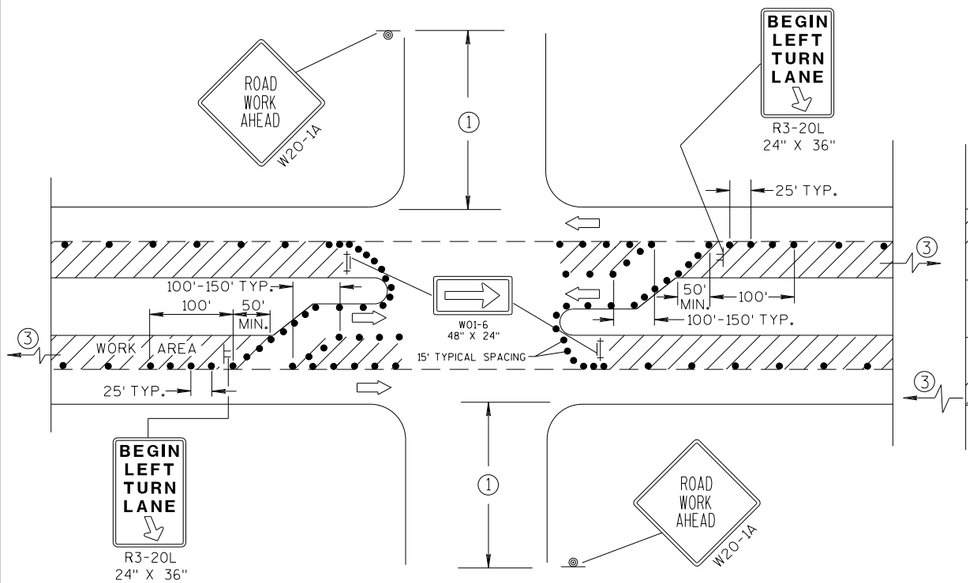
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

(1) 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35-40 MPH.
200' IF 25-30 MPH.

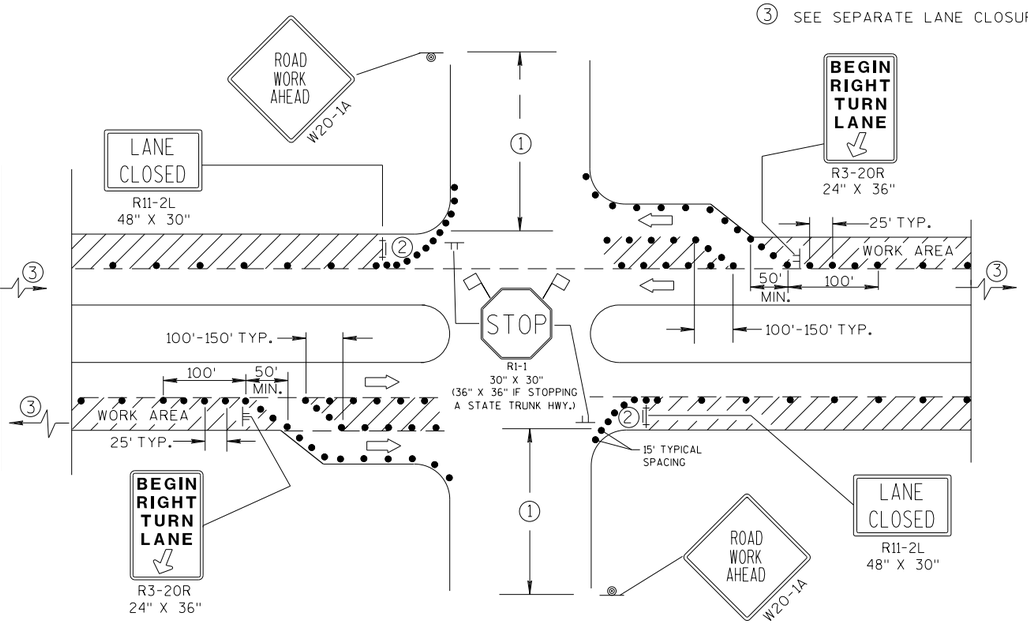
(2) ALSO USE BARRICADE AND 15-FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS.

(3) SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.



DETAIL C

FOR LEFT LANE CLOSURE AT INTERSECTION OR MEDIAN OPENING (WITH LEFT TURN BAY OPEN)



DETAIL D

FOR RIGHT LANE CLOSURE AT INTERSECTION (WITH RIGHT TURN BAY OPEN)

LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT (5' MIN. MOUNTING HEIGHT)
- TYPE III BARRICADE WITH ATTACHED SIGN AND TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- FLAGS, 16" X 16" MIN., (ORANGE)
- WORK AREA

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

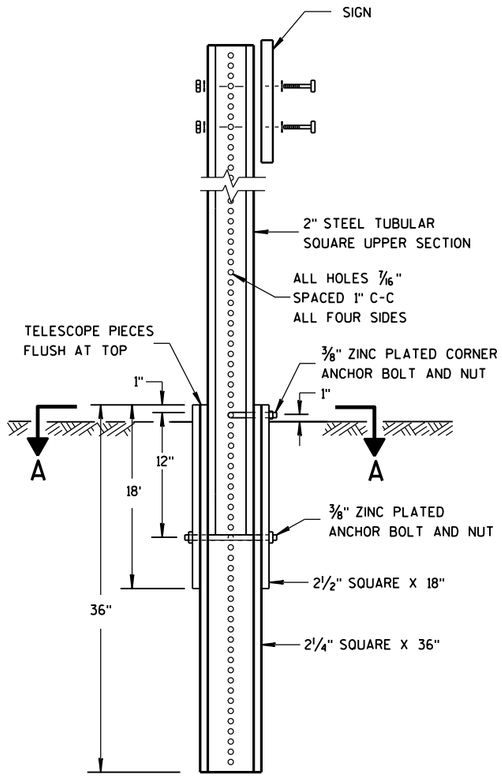
7/2018
DATE

/S/ Andrew Heldtke
WORK ZONE ENG

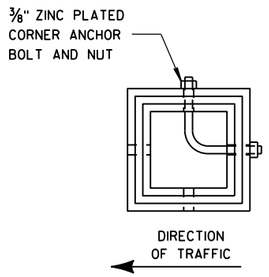
FHWA

6

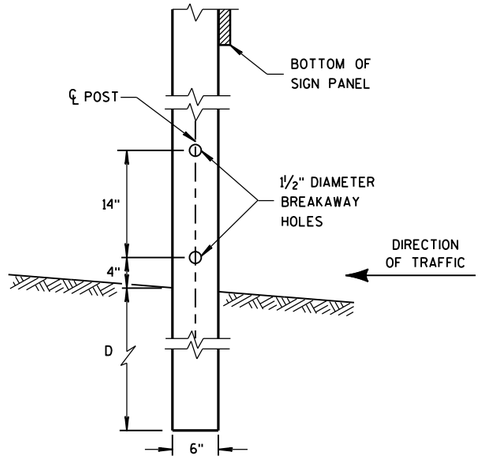
S.D.D. 15 D 21-6



DETAIL OF TUBULAR STEEL SIGN POST



SECTION A-A



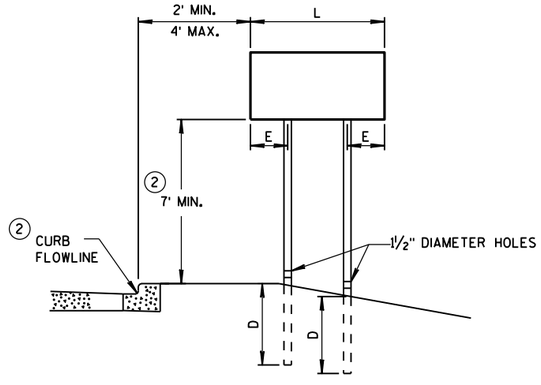
4"x6" WOOD POST MODIFICATION

GENERAL NOTES

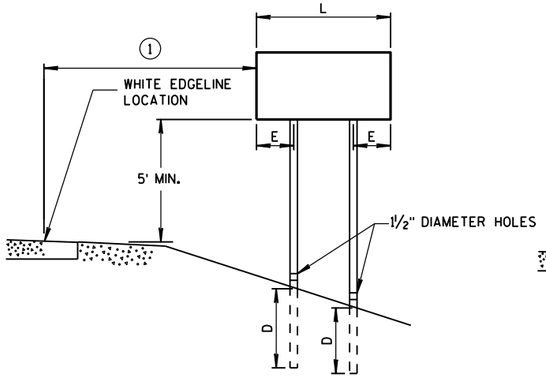
(1) 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.

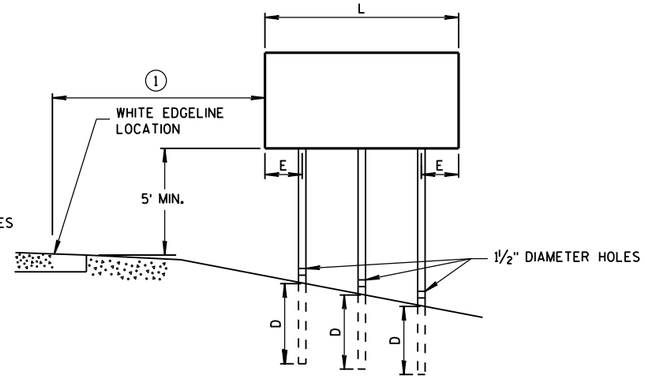
(2) THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.

(3) FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.



URBAN AREA





RURAL AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

SEE NOTE (3)

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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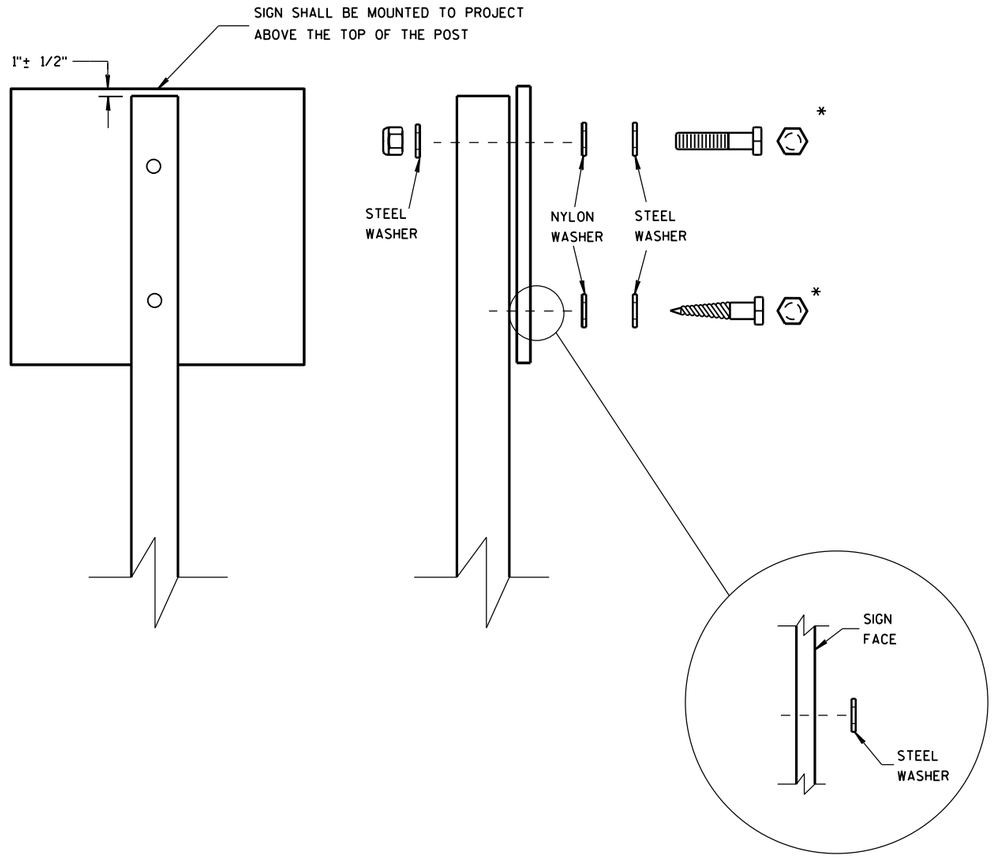
S.D.D. 15 D 38-2a



WASHER PLACEMENT WHEN SIGN HAS OTHER THAN TYPE H OR TYPE F FACE

NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3

B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3"

MACHINE BOLTS - 5/16" X 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

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S.D.D. 15 D 38-2b

S.D.D. 15 D 38-2b

ATTACHMENT OF SIGNS TO POSTS

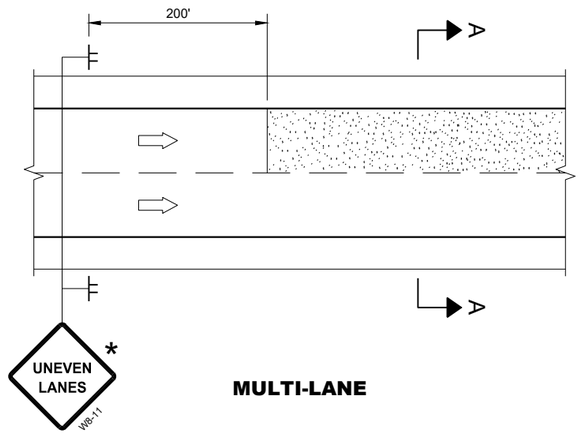
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

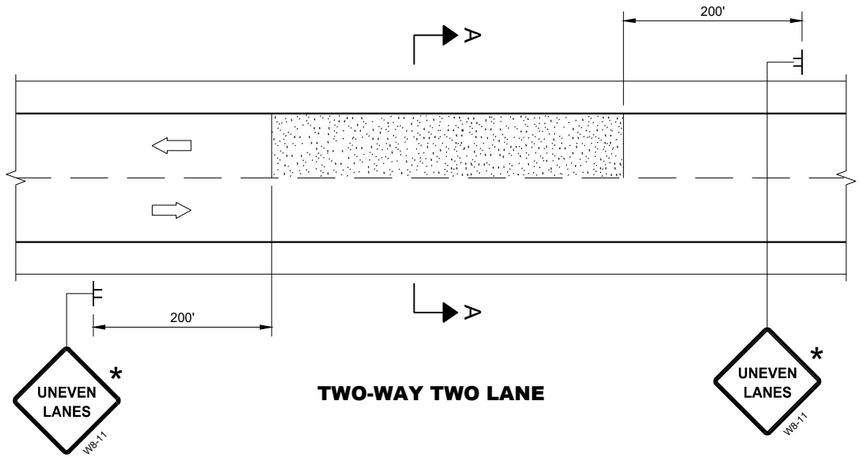
June 2017
DATE

/S/ Andrew Heldtke
WORK ZONE ENG

FHWA



MULTI-LANE



TWO-WAY TWO LANE

GENERAL NOTES

FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

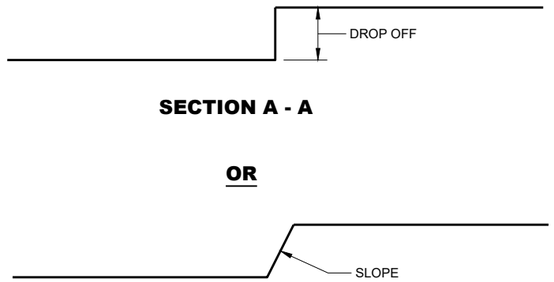
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.

(1) USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

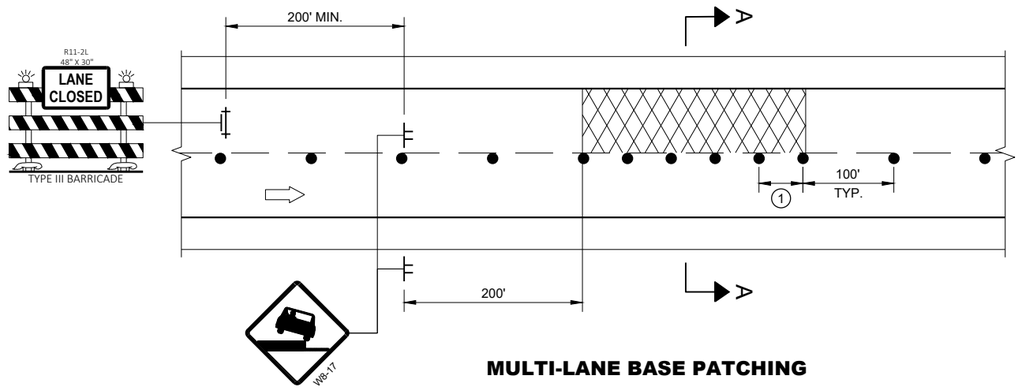
LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE



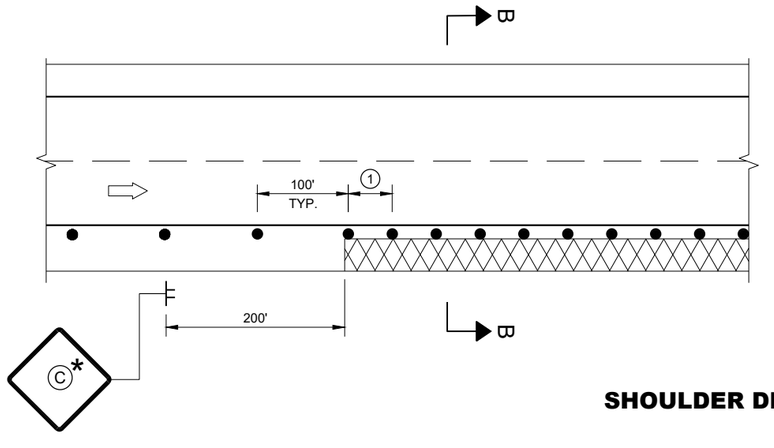
SECTION A - A

SECTION A - A

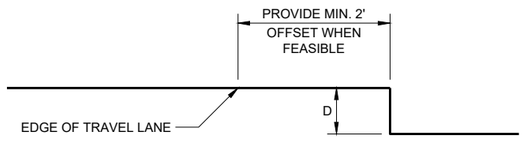


MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

TRAFFIC CONTROL, DROP-OFF SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15D39 - 02

6

SDD 15D39 - 02

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

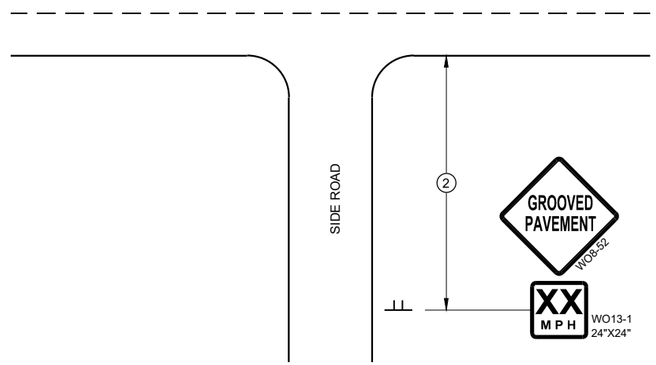
(1) PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.

(2) PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

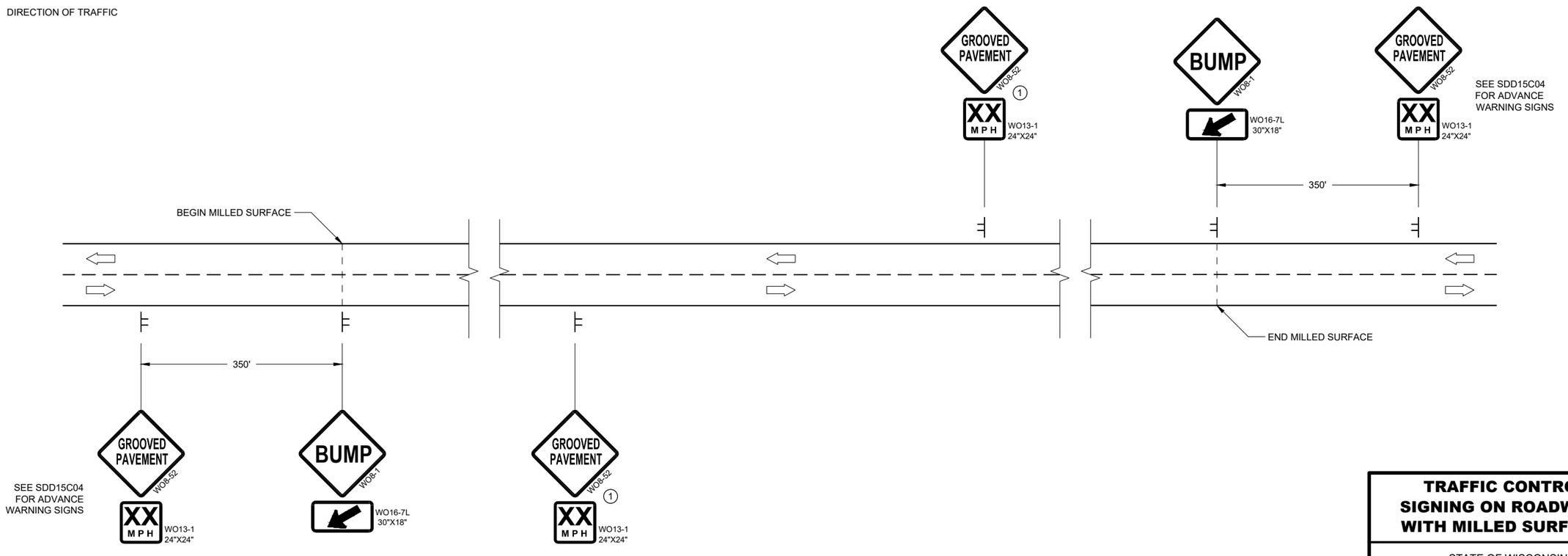
LEGEND

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
August 2019
DATE

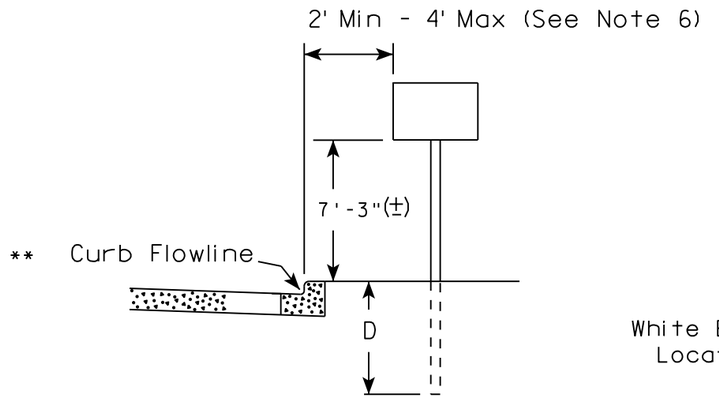
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

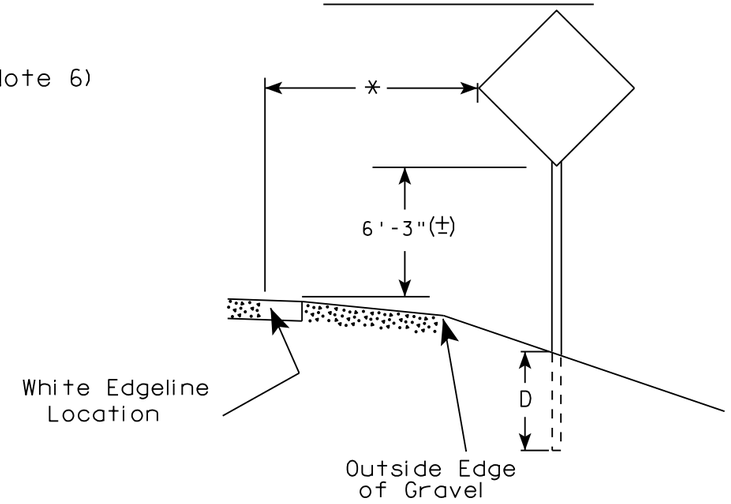
6

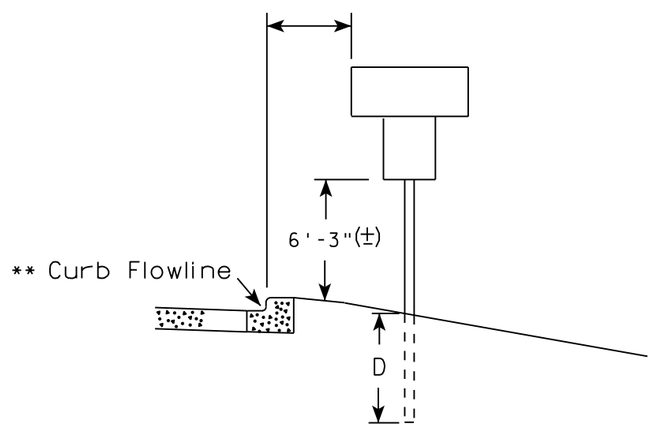
SDD 15D44 - 01

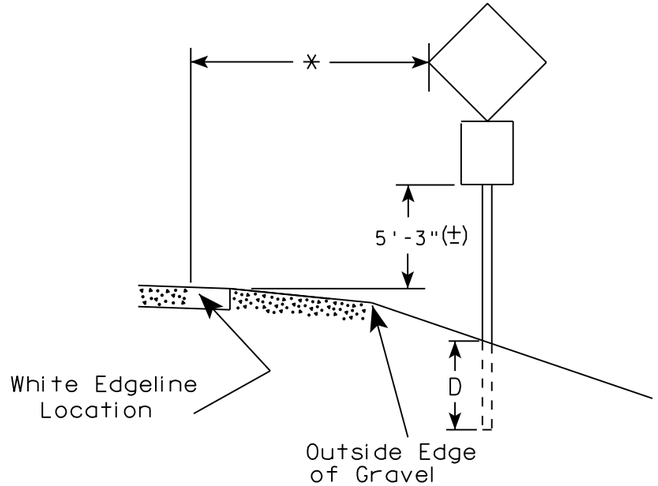
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. J-Assemblies are considered to be one sign for mounting height.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

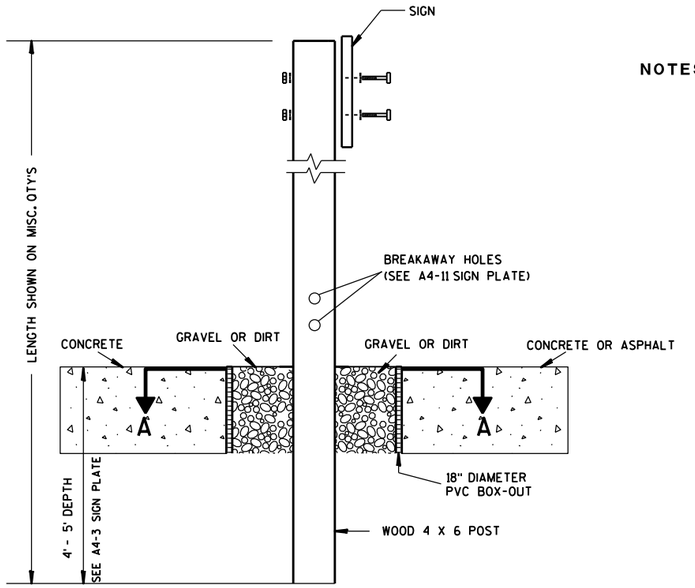
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-3.21

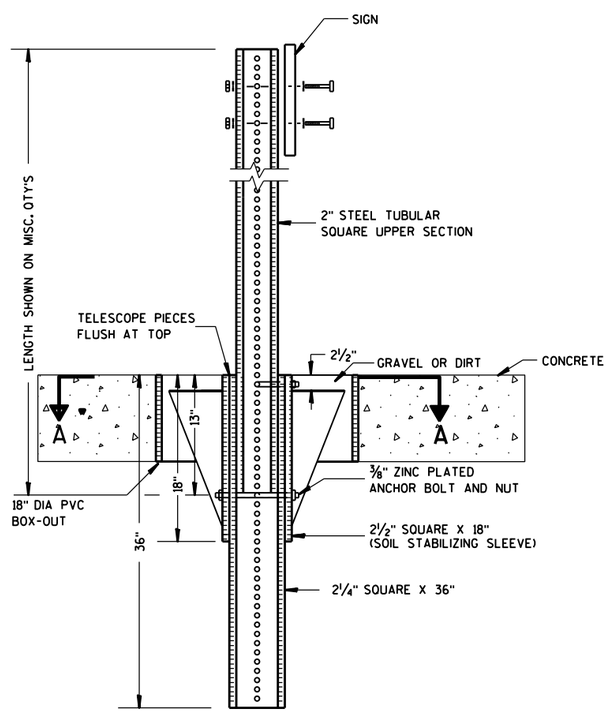
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 123	E



ELEVATION VIEW

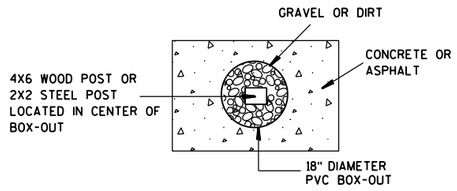
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

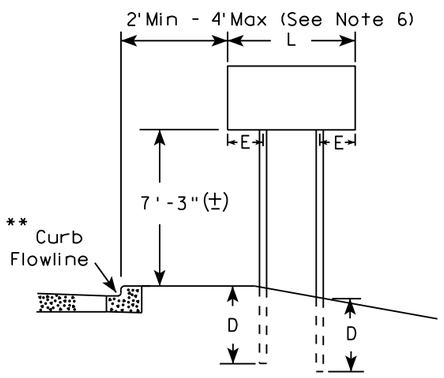
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

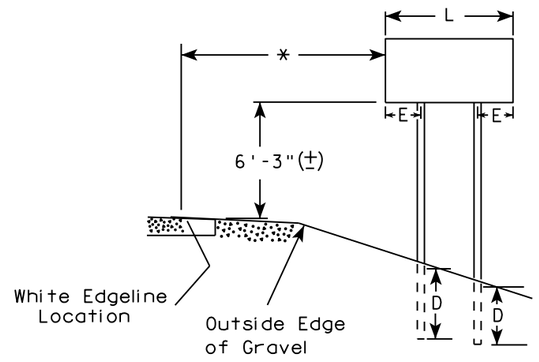
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

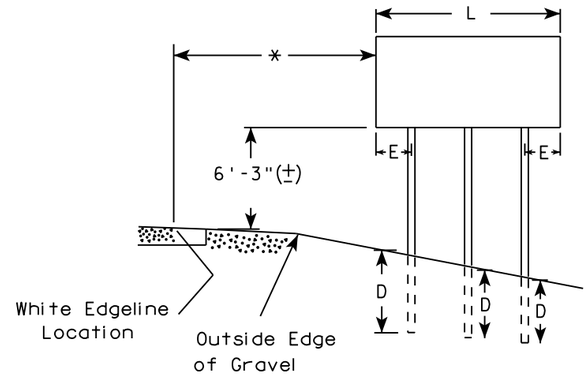
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

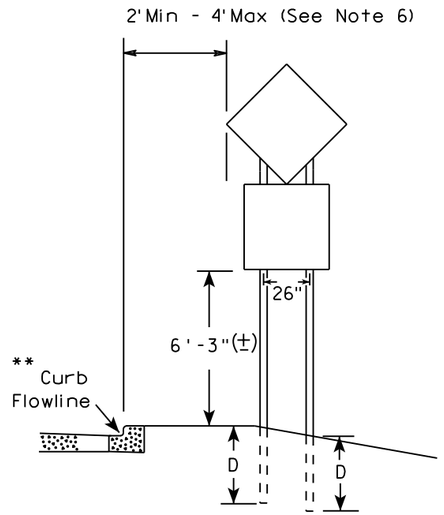
URBAN AREA



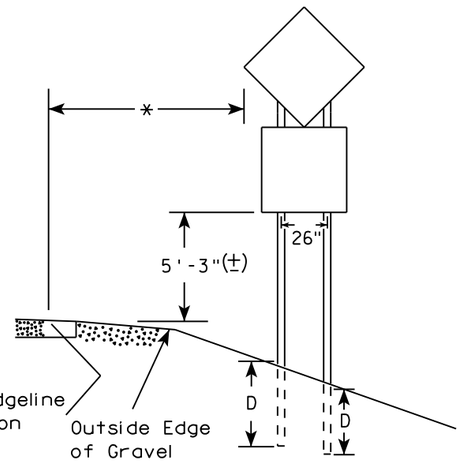
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

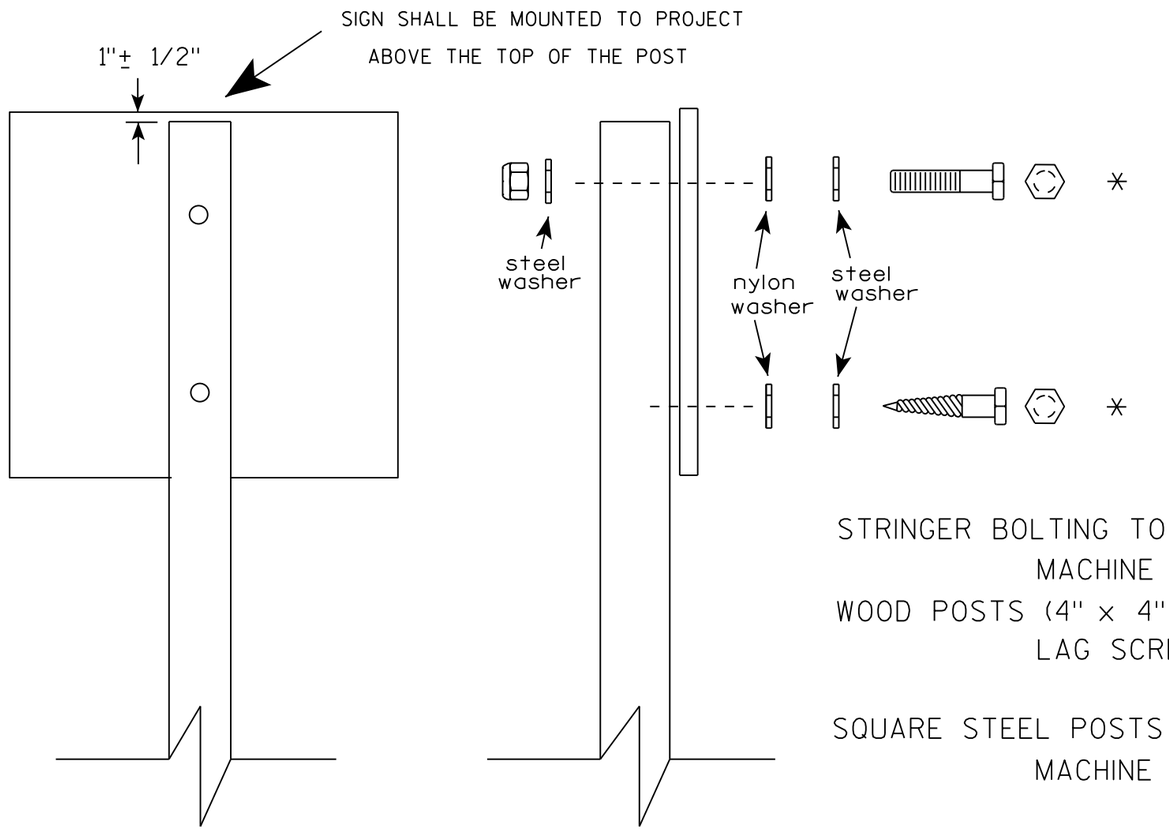
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 125	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/11/16 PLATE NO. A4-8.8

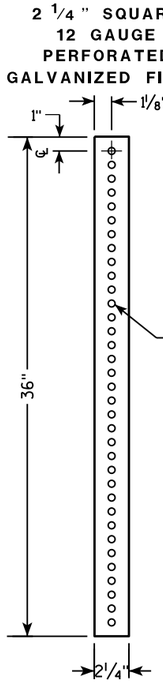
7

PROJECT NO: SHEET NO: E

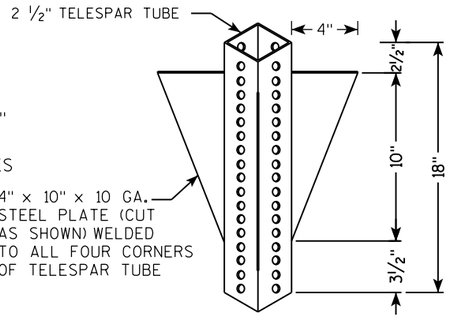
FILE NAME : C:\CADfiles\Projects\tr_stdplate\A48.DGN PLOT DATE : 11-AUG-2016 11:35 PLOT BY : $$ plotuser $$

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

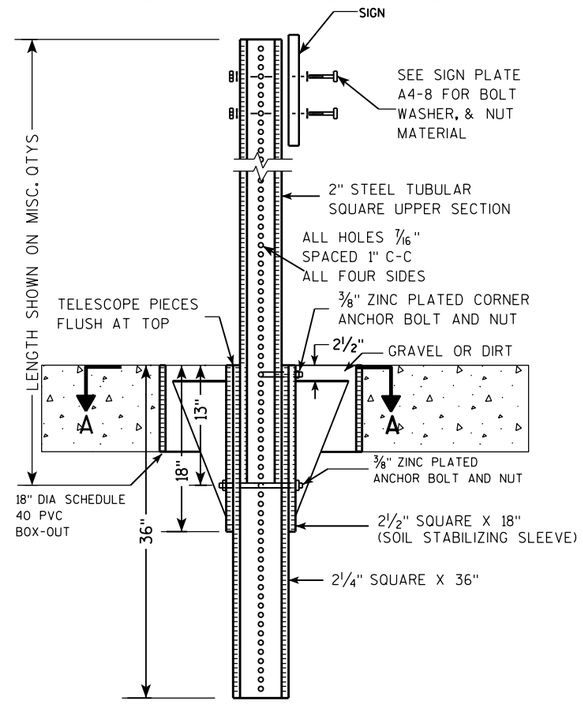
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



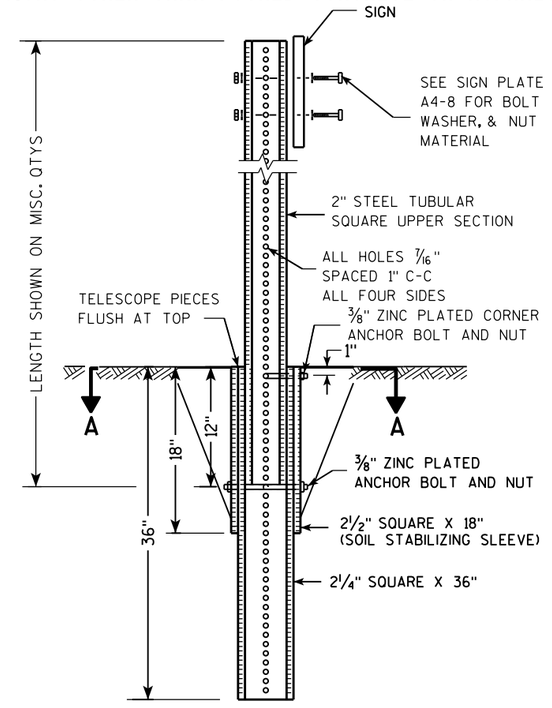
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

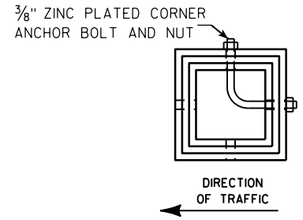


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

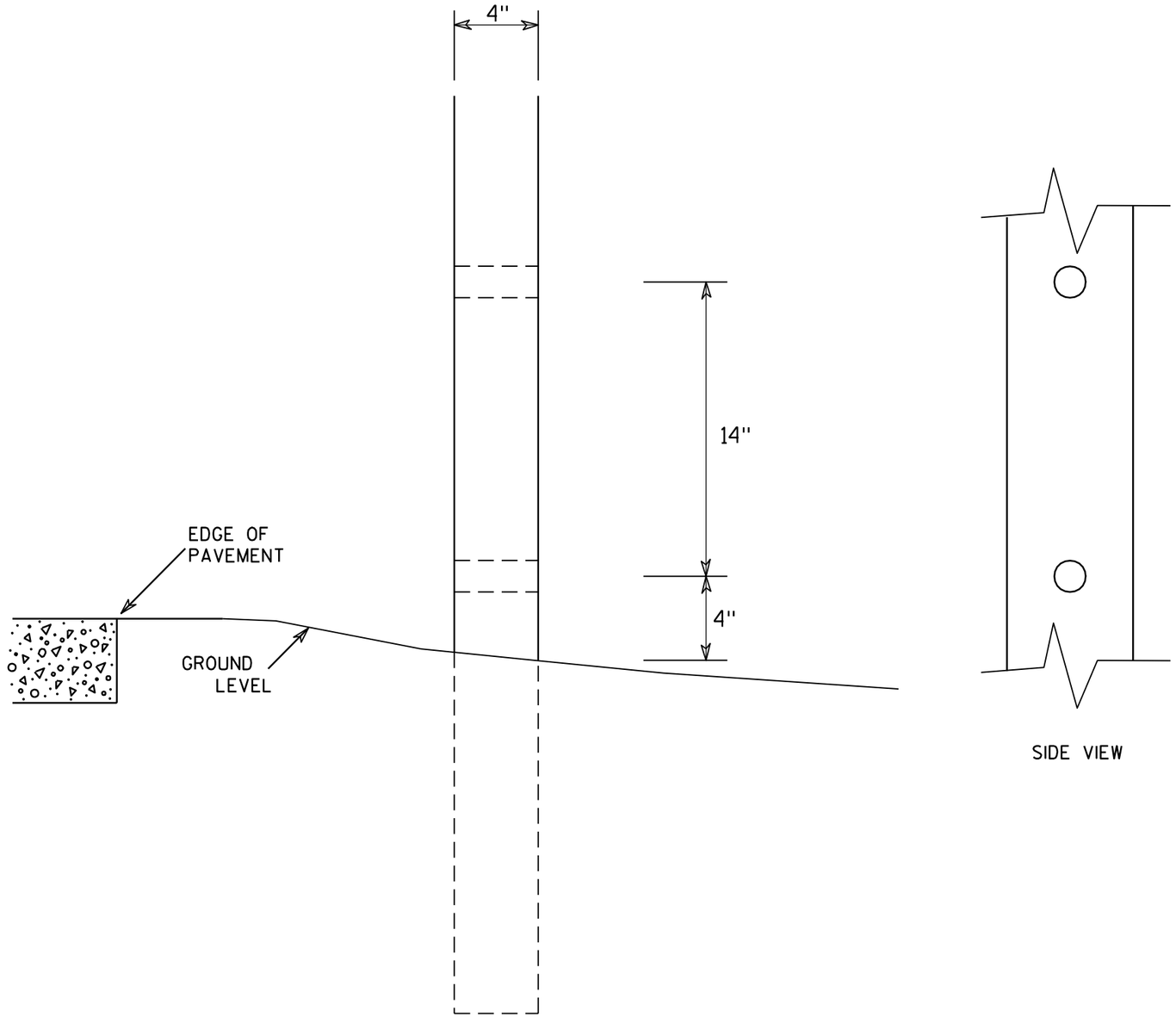
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

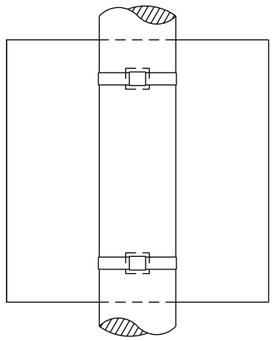
APPROVED Chester J Spang for State Traffic Engineer

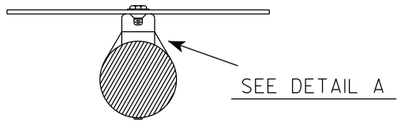
DATE 3/27/97 PLATE NO. A4-11.2

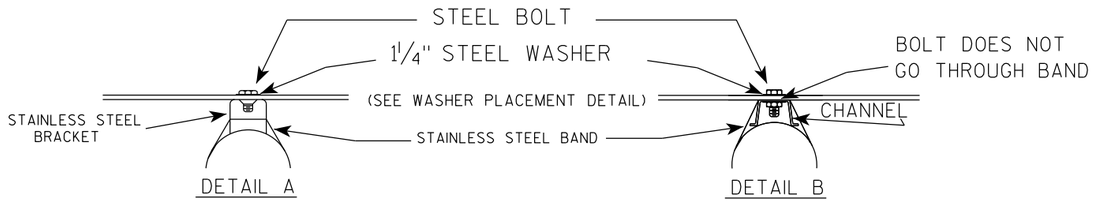
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 128	E

BANDING

SINGLE SIGN



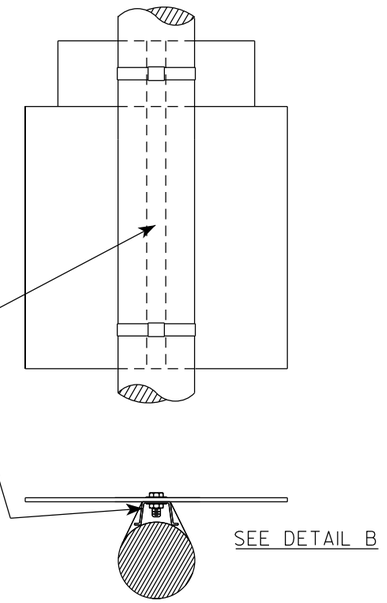




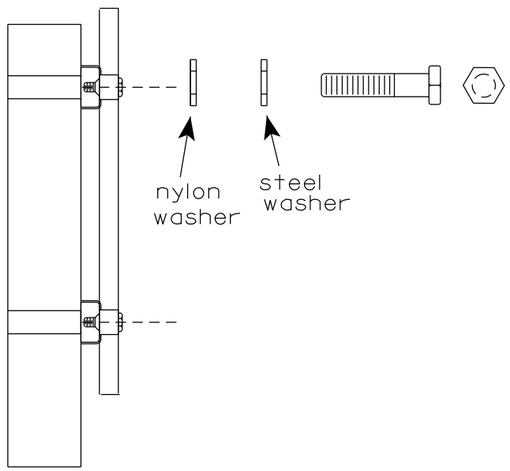
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X ⅜" I.D. X 1/16" STEEL
1-1/4" O.D. X ⅜" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

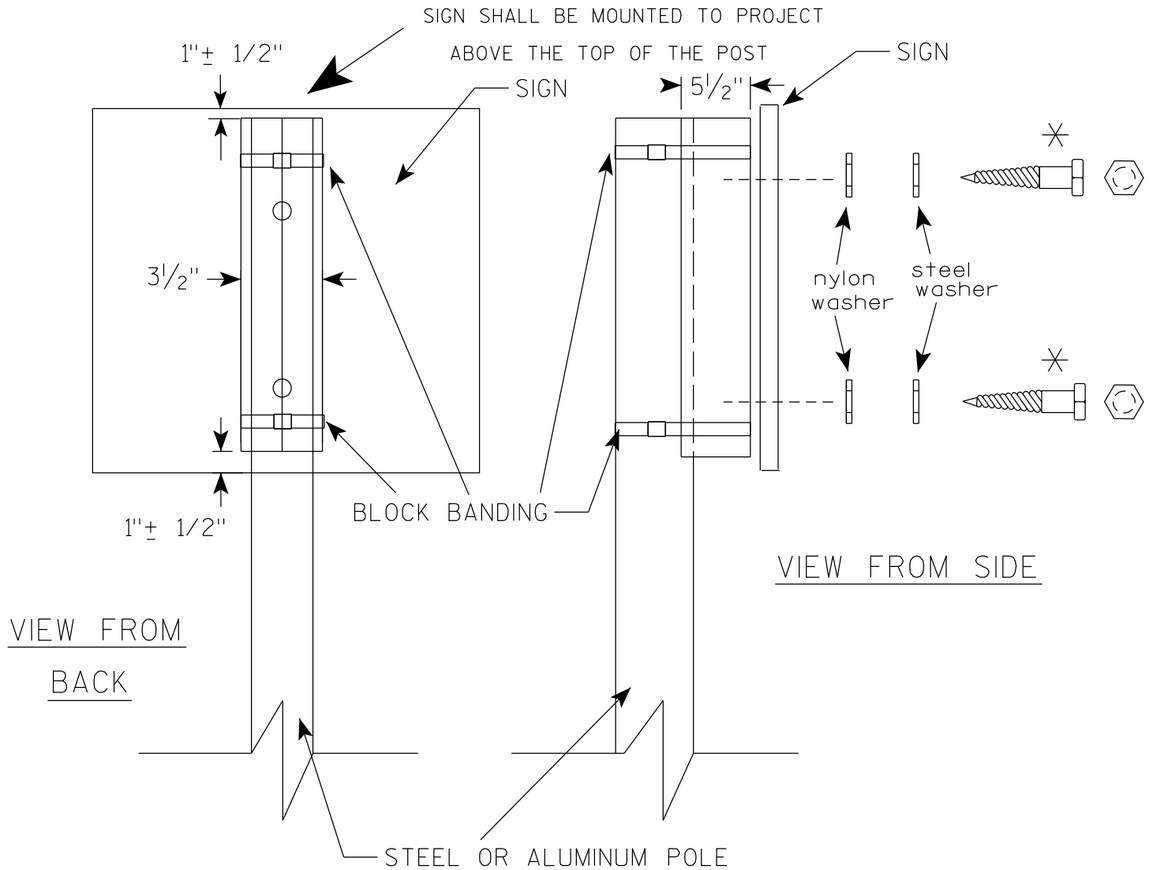
WISCONSIN DEPT OF TRANSPORTATION

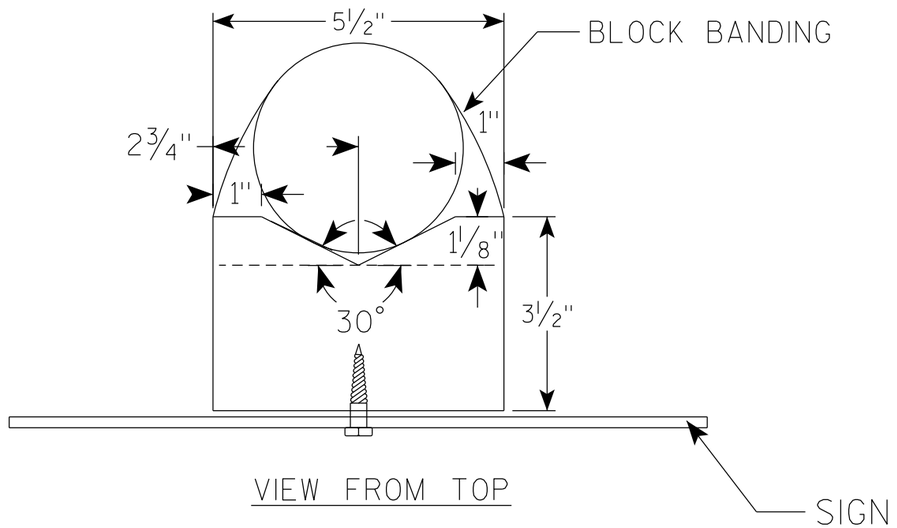
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 129	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

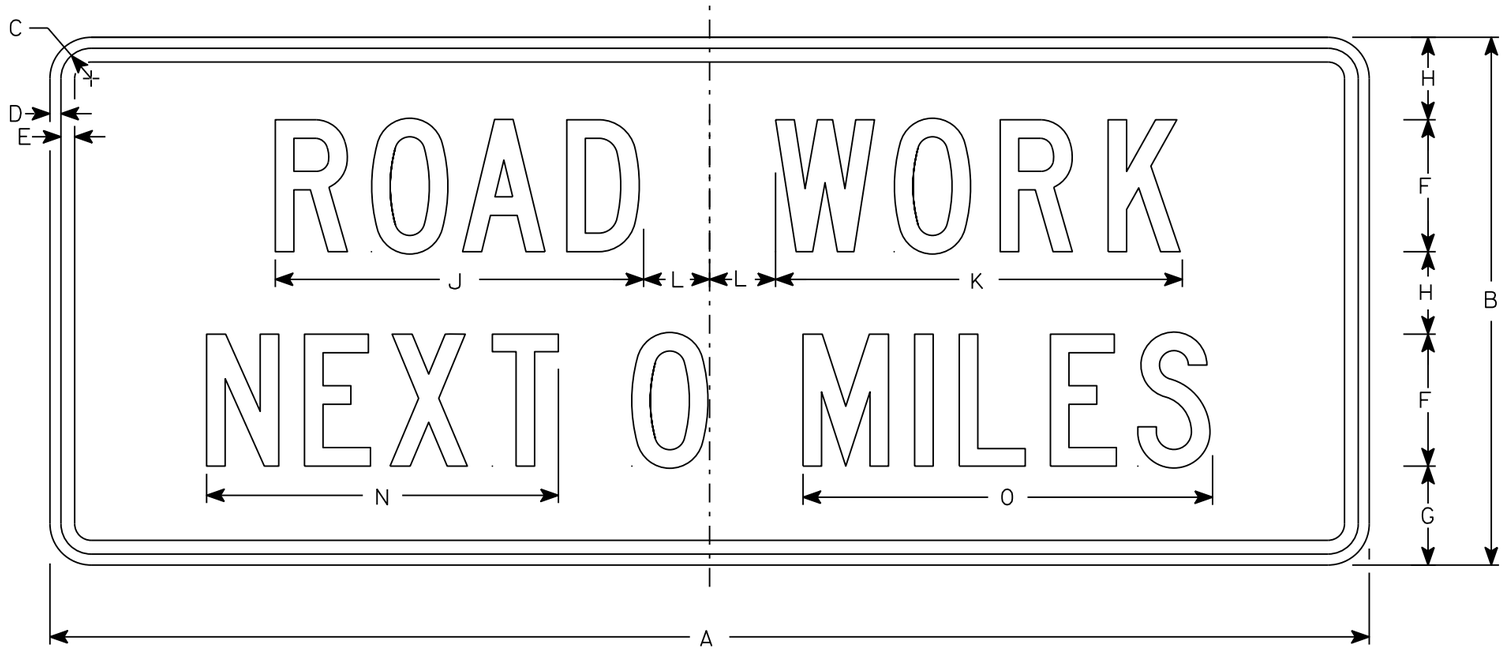
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-10.2

PROJECT NO:

SHEET NO: 130 E



G20-1

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4		16 3/4	18 1/2	3		16	18 5/8												10
3																											
4	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4		16 3/4	18 1/2	3		16	18 5/8												10
5																											

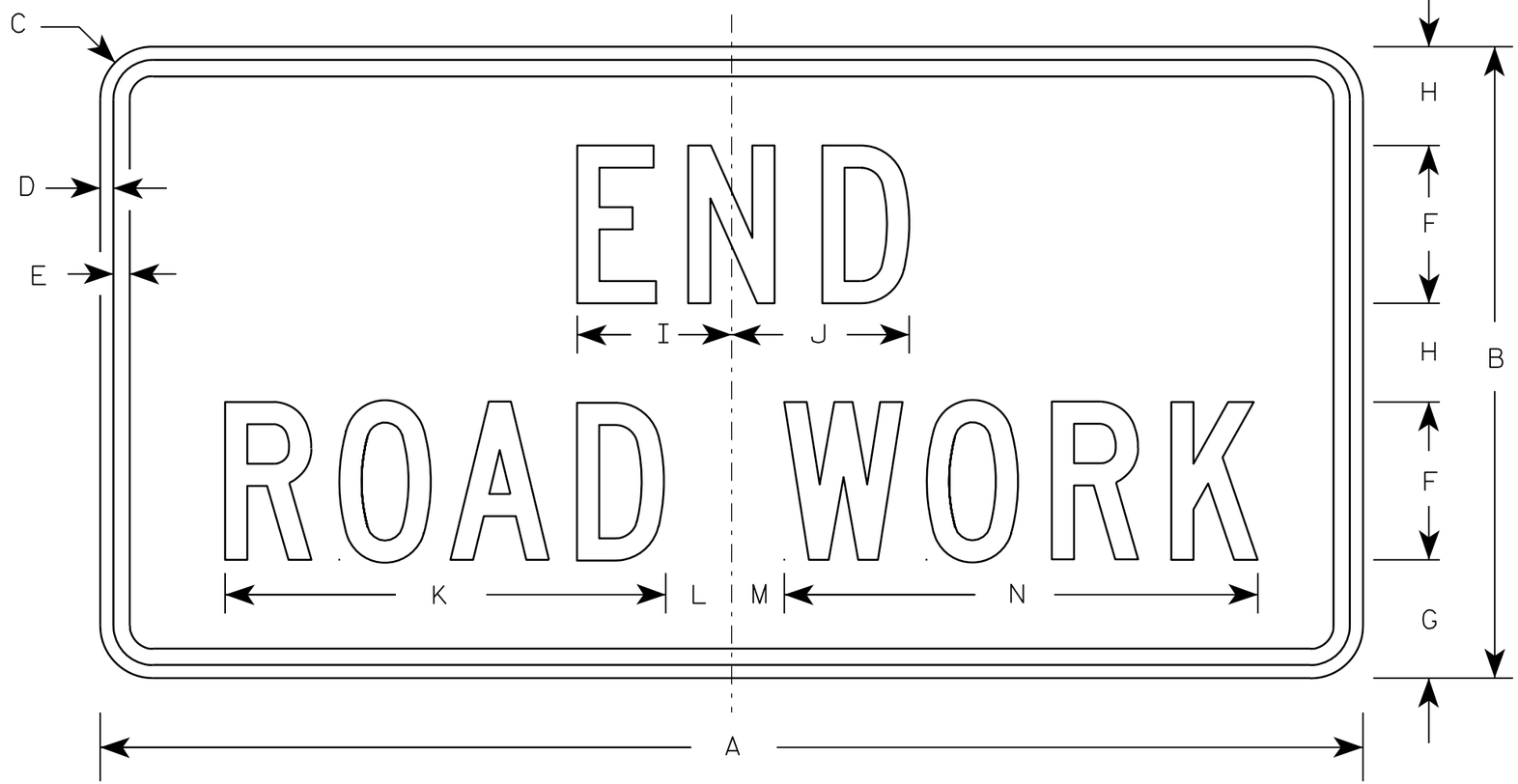
STANDARD SIGN
G20-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/14/17 PLATE NO. G20-1.8

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 131	E



G20-2A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

Metric equivalent for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

STANDARD SIGN
G20-2A

WISCONSIN DEPT OF TRANSPORTATION

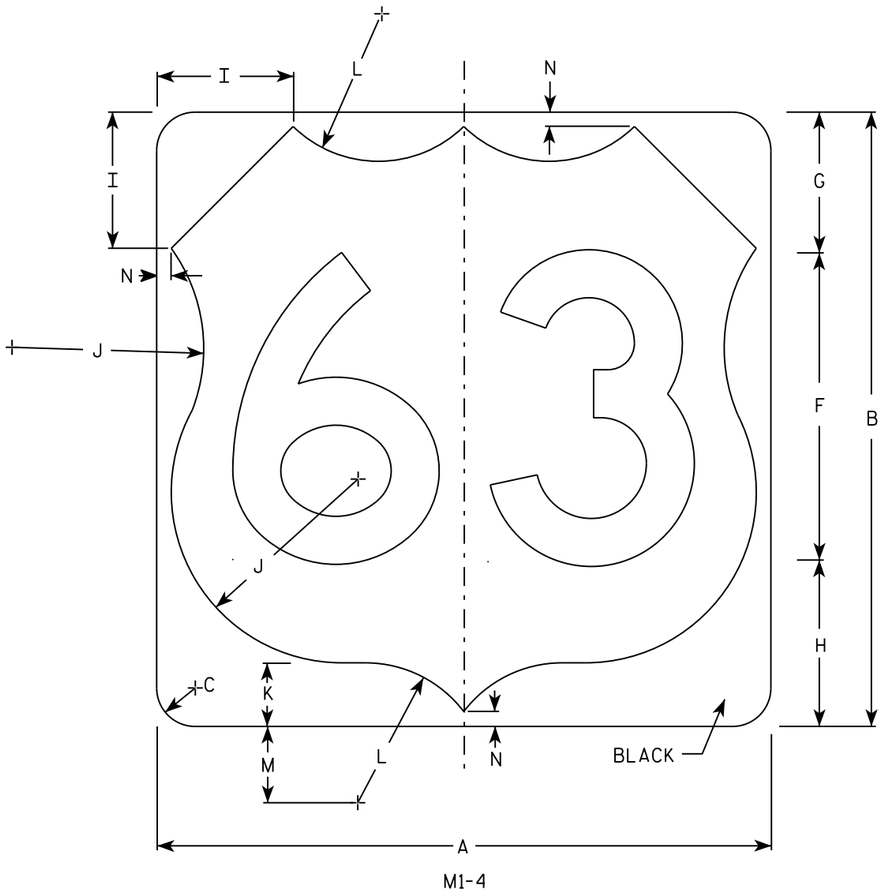
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

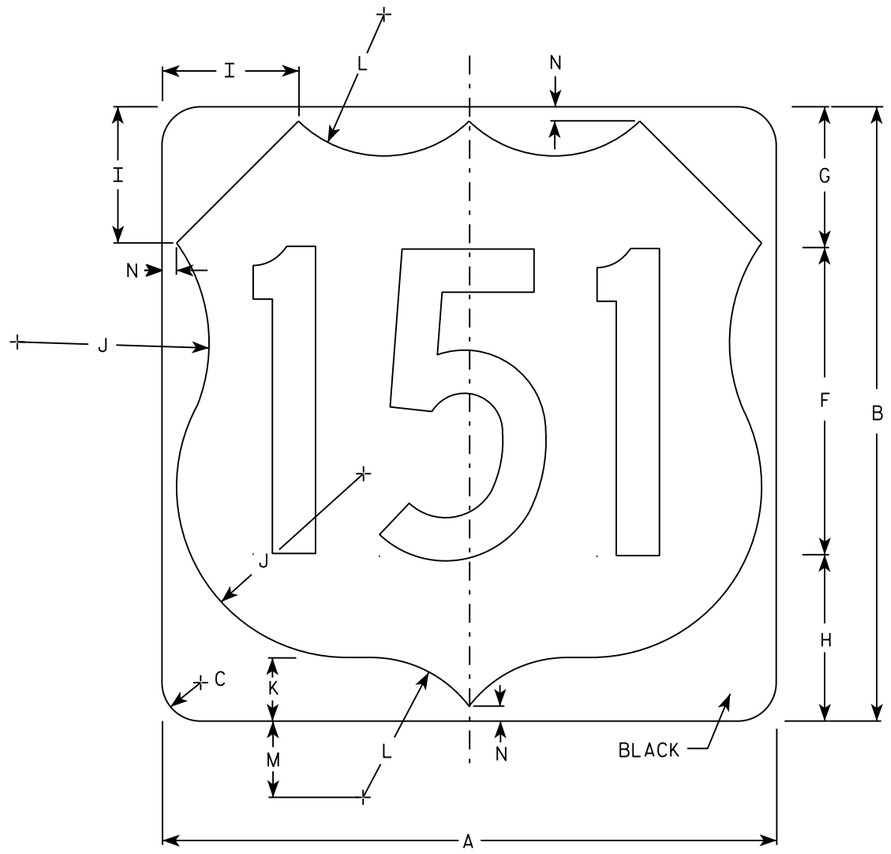
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 132	E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M1-4



M1-4

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area Sq. ft.
1																											
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0

USH MARKER
M1-4 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-4.10

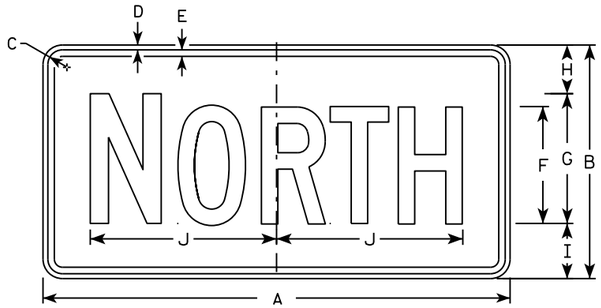
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 133	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M14.DGN PLOT DATE : 16-MAR-2018 14:06 PLOT BY : $$...plotuser...$$ PLOT NAME : PLOT SCALE : 5.918903:1.000000

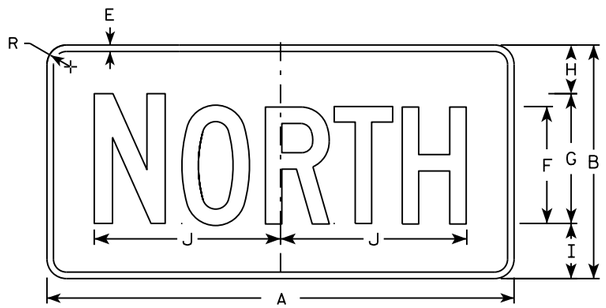
WISDOT/CADDS SHEET 42

7

7



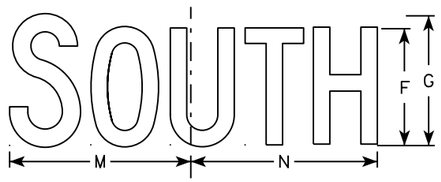
M3-1
MM3-1
MP3-1



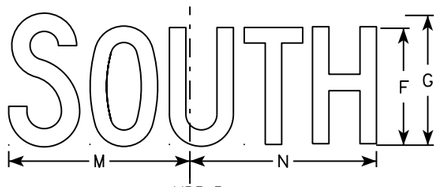
MB3-1
MK3-1
MN3-1

M3-2
MM3-2
MP3-2

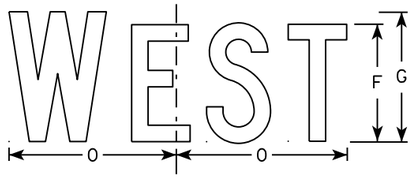
MB3-2
MK3-2
MN3-2



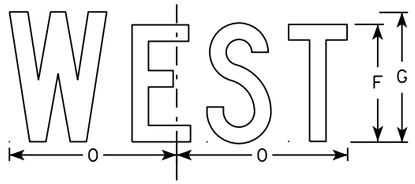
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
   Message - Black
   MB3-1 thru MB3-4 Background - Blue
   Message - White
   MK3-1 thru MK3-4 Background - Green
   Message - White
   MM3-1 thru MM3-4 Background - White
   Message - Green
   MN3-1 thru MN3-4 Background - Brown
   Message - White
   MP3-1 thru MP3-4 Background - White
   Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thur M3-4
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew P Rauch for State Traffic Engineer

DATE 10/15/15 PLATE NO M3-1.14

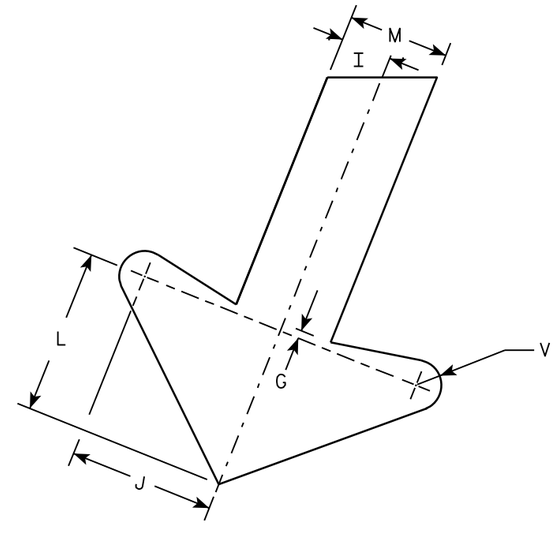
PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M31.DGN PLOT DATE : 01-DEC-2015 17:54 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 11.675051:1.000000

R3-20R

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	12 3/4	12 1/2		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

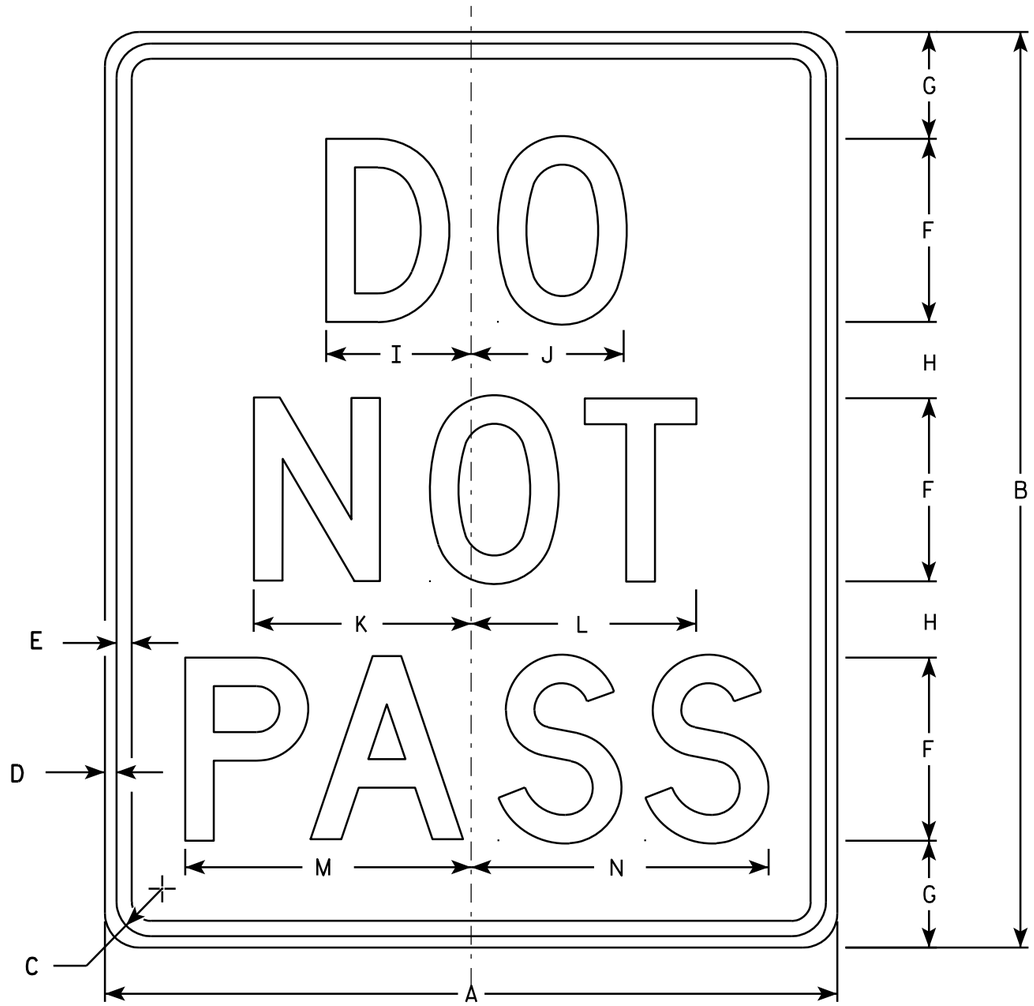
STANDARD SIGN
R3-20R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/18/10 PLATE NO. R3-20R.6

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 135 | E



R4-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	4	3 1/2	2 1/2	3 1/8	3 1/4	4 3/4	4 7/8	6 1/4	6 1/2													3.0
2S	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
2M	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
3																											
4	36	48	1 5/8	5/8	3/4	8	7	5	6 1/4	6 5/8	9 1/2	9 3/4	12 1/2	13													12.0
5	48	60	2 1/4	3/4	1	10	8	7	7 3/4	8 3/8	11 7/8	12 1/4	15 5/8	16 1/4													20.0

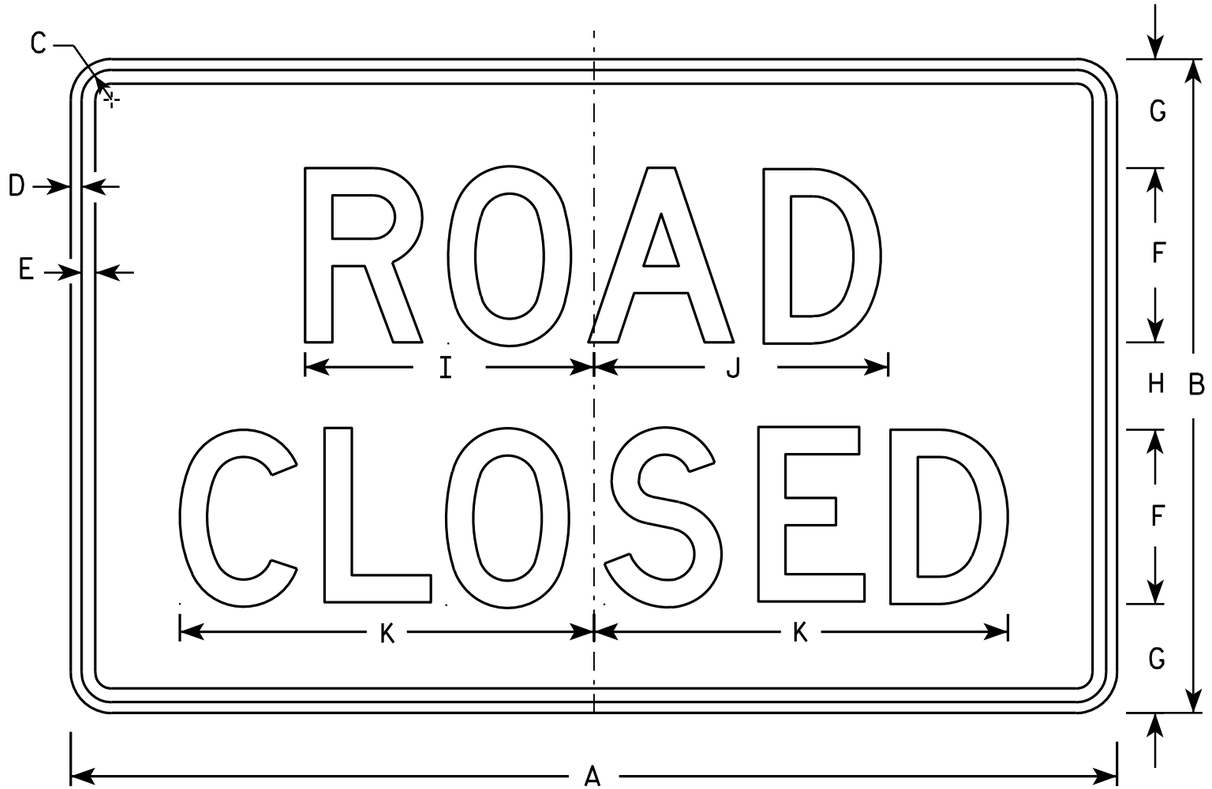
STANDARD SIGN
R4-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-1.7

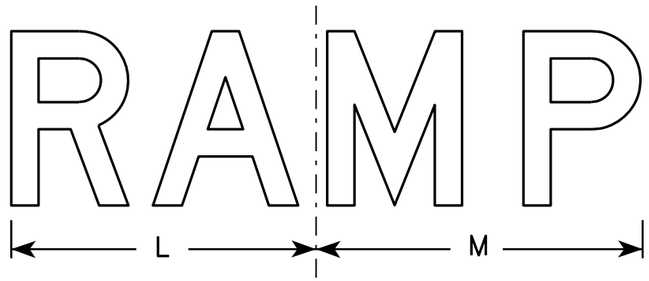
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 136 | E



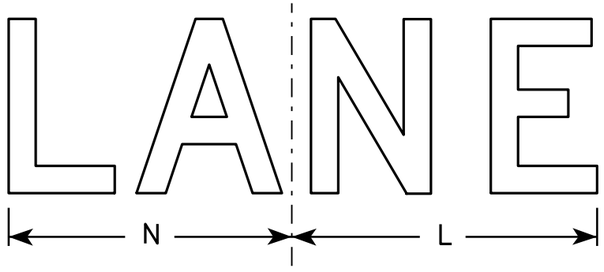
R11-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.



R11-2R



R11-2L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0

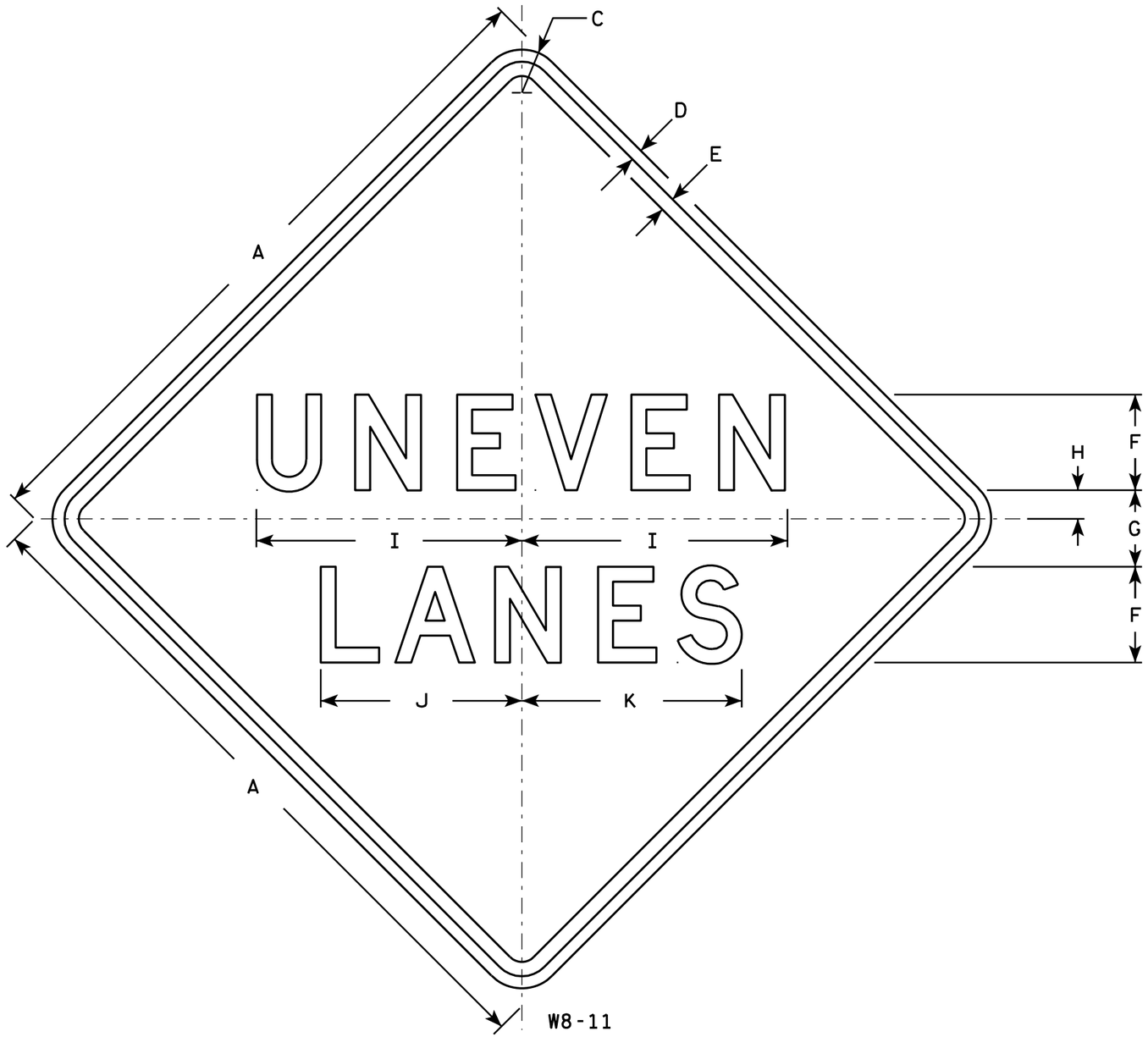
STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-2.10

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 137	E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5/8	5/8	3/4	5	4	1 1/2	13 7/8	10 1/2	11 1/2																9.0
2M	36		1 5/8	5/8	3/4	5	4	1 1/2	13 7/8	10 1/2	11 1/2																9.0
3																											
4	36		1 5/8	5/8	3/4	5	4	1 1/2	13 7/8	10 1/2	11 1/2																9.0
5	48		2 1/4	3/4	1	7	5	2	18 1/2	14	15 3/8																16.0

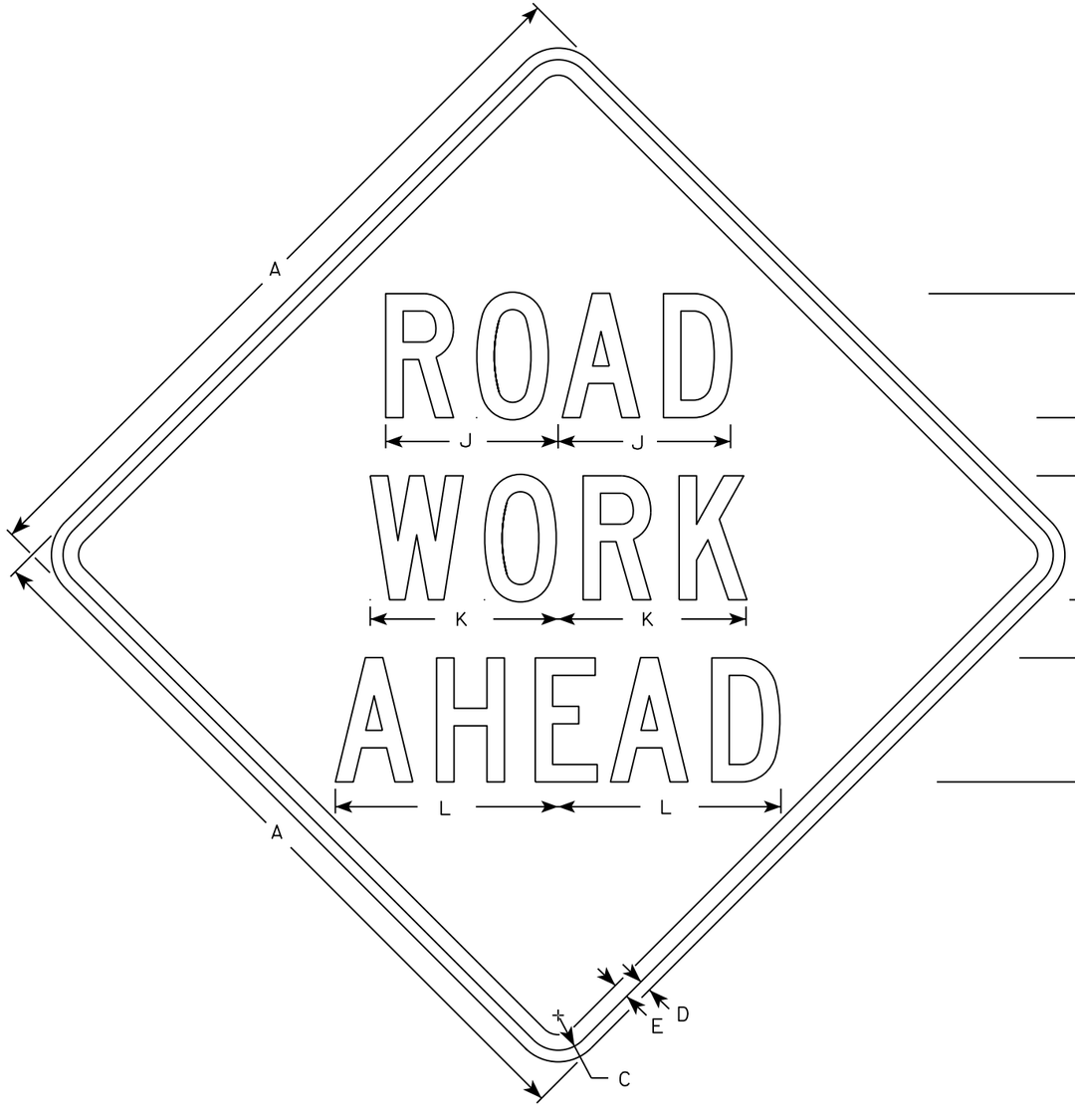
STANDARD SIGN
W8-11

WISCONSIN DEPT OF TRANSPORTATION

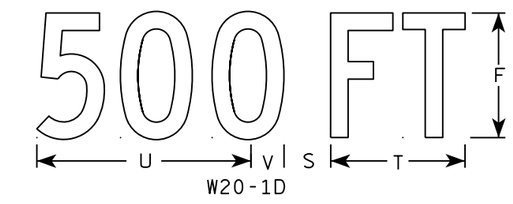
APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 3/22/11 PLATE NO. W8-11.4

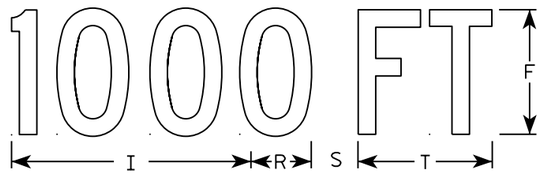
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 138	E



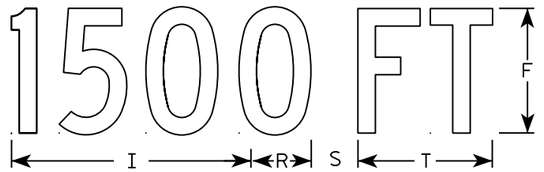
W20-1A



W20-1D

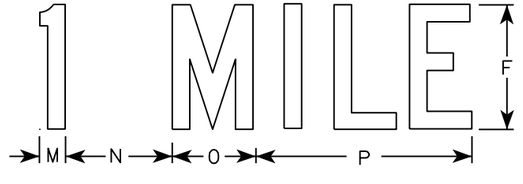


W20-1C

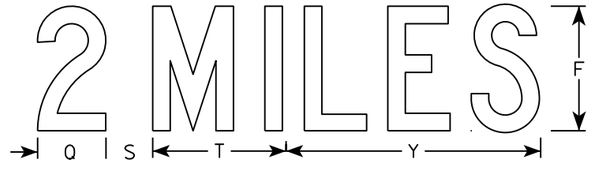


W20-1B

W20-1G



W20-1F



W20-1E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

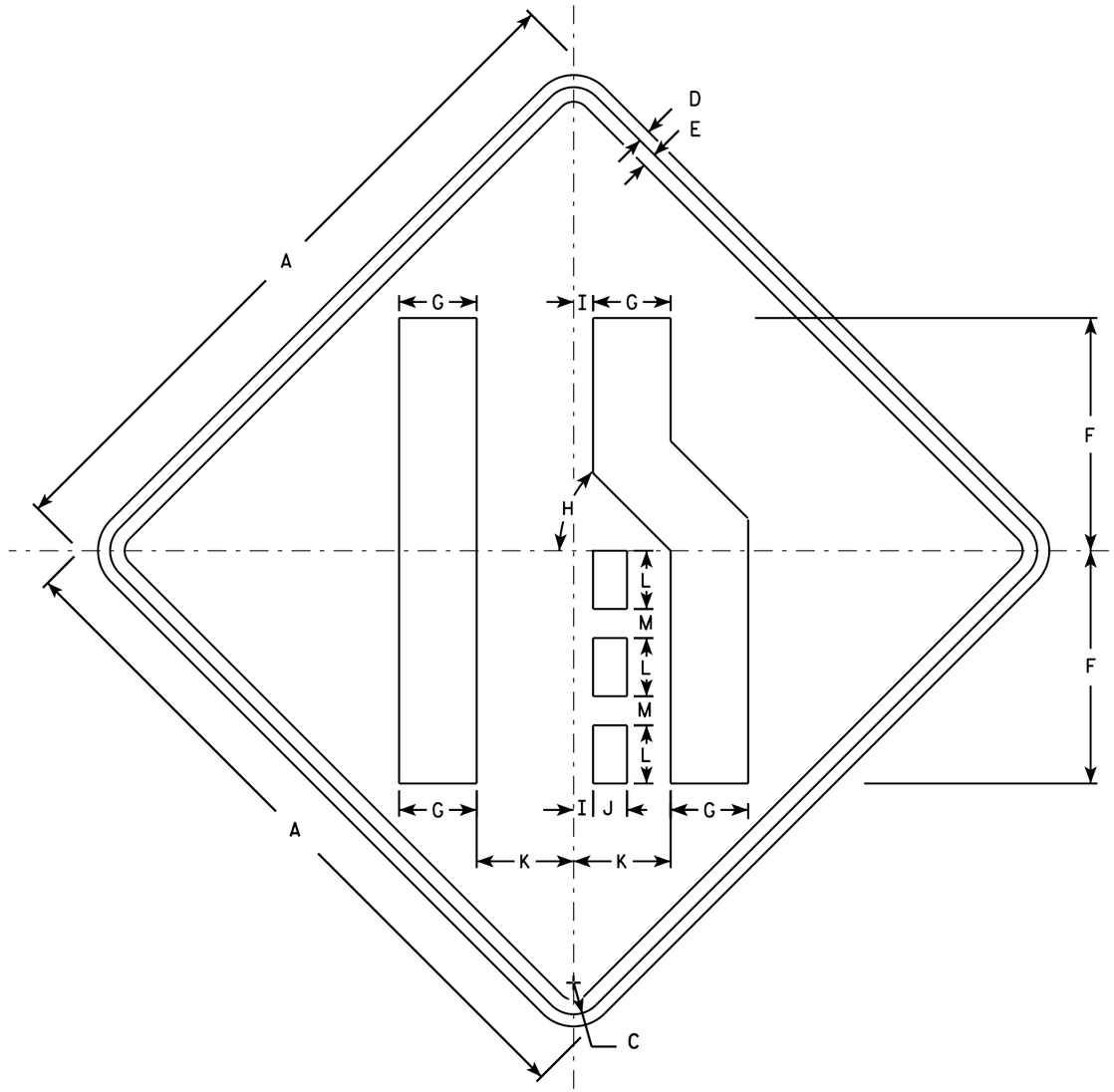
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/07/15 PLATE NO. W20-1.10

PROJECT NO:

SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W201.DGN PLOT DATE : 01-DEC-2015 18:24 PLOT BY : $$ plotuser $$



W04-2R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W04-2L is the same as W04-2R except the symbol is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
2M	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
3	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
4	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0

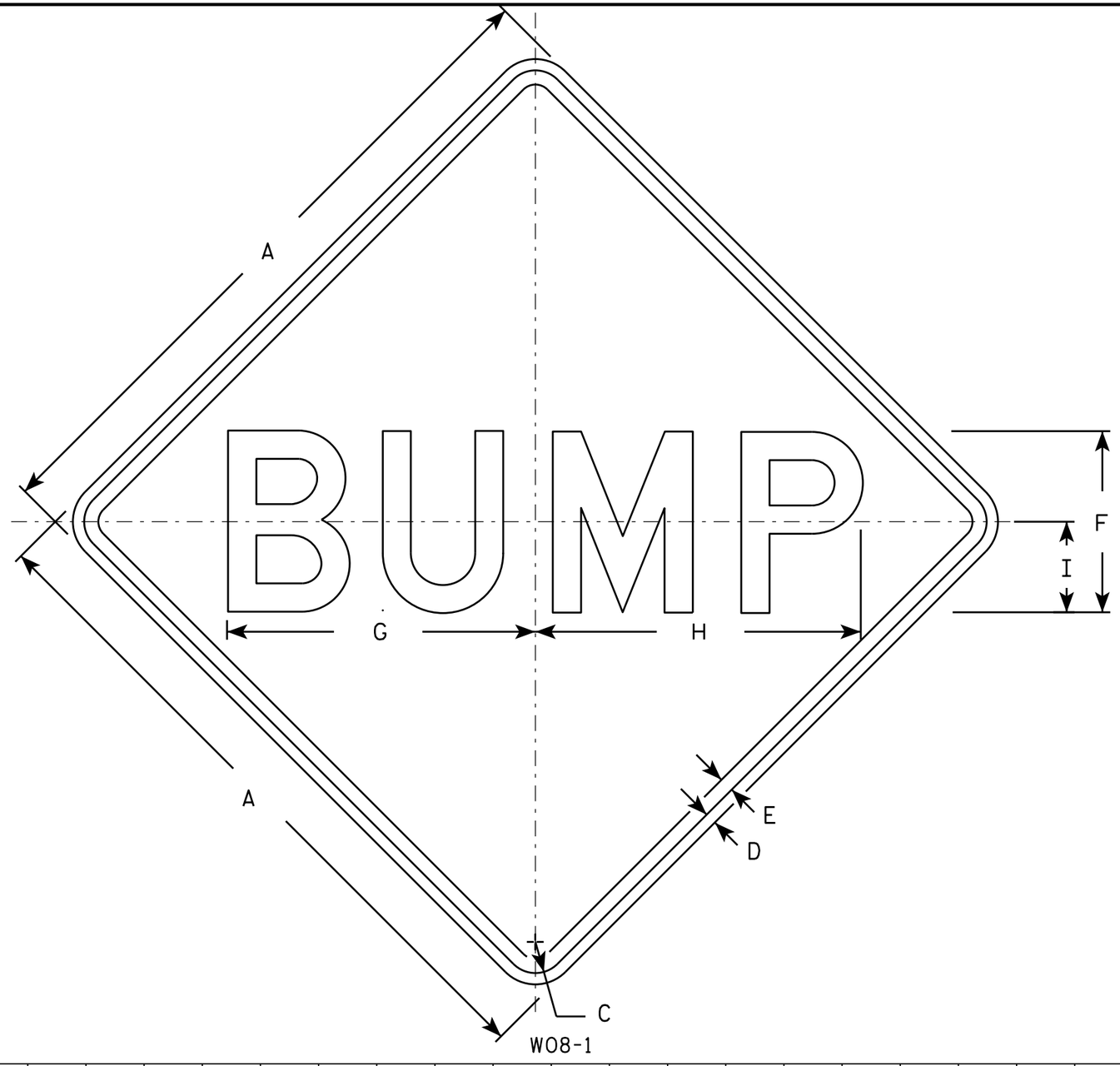
STANDARD SIGN
W04-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE 140 W04-2.1

PROJECT NO: | SHEET NO: E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	10	16 7/8	17 7/8	5																		9.0
2S	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
2M	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
3	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
4	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
5	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0

STANDARD SIGN
W08-1

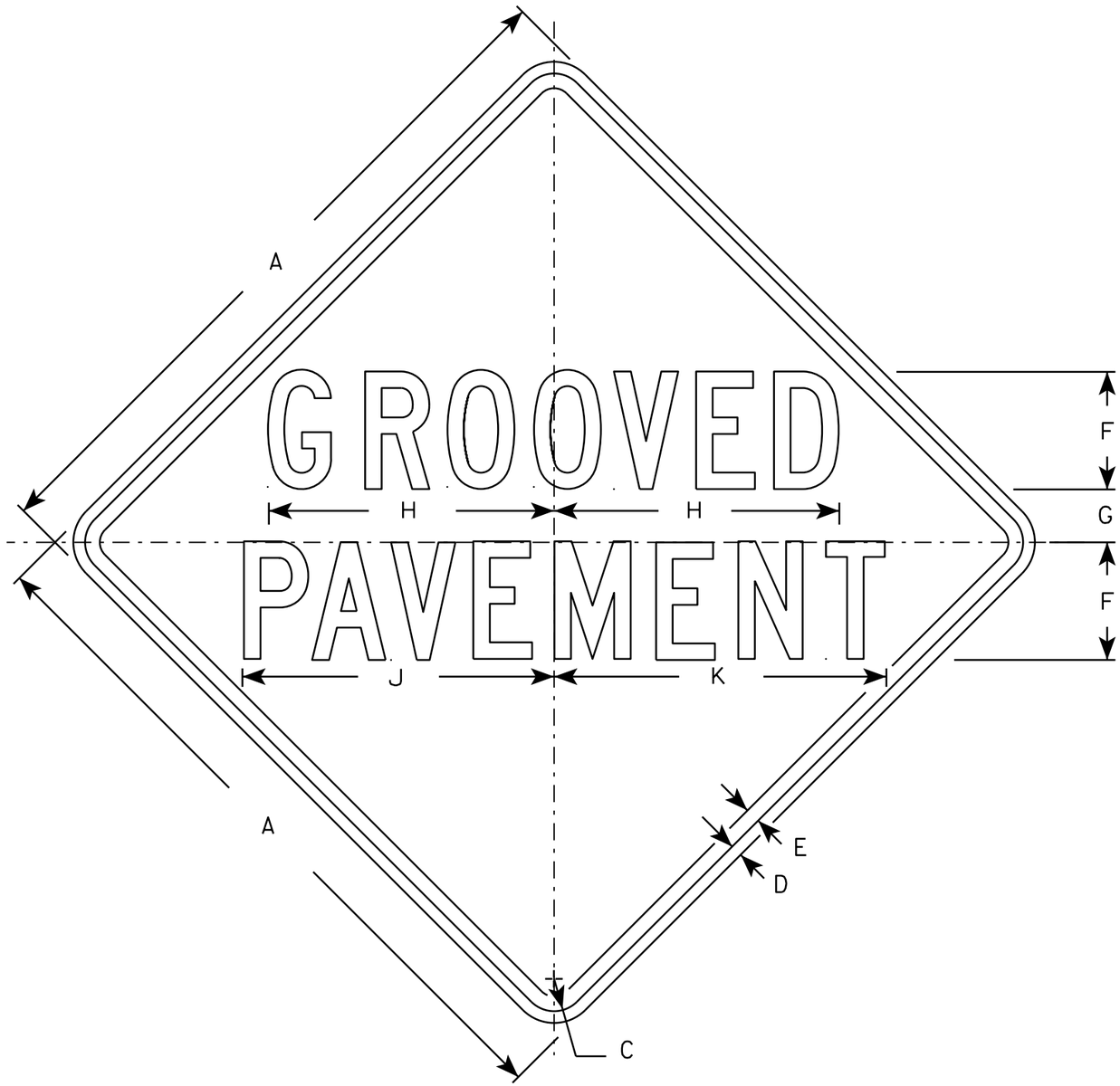
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE 141 W08-1.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W081.DGN PLOT DATE : 20-NOV-2013 12:24 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 6.688833:1.000000 WISDOT/CADDS SHEET 42



W08-52

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	2 5/8	14 1/2		15 7/8	17																9.0
2S	48		2 1/4	3/4	1	8	3 1/2	19 3/8		21 1/4	22 5/8																16.0
2M	48		2 1/4	3/4	1	8	3 1/2	19 3/8		21 1/4	22 5/8																16.0
3	48		2 1/4	3/4	1	8	3 1/2	19 3/8		21 1/4	22 5/8																16.0
4	48		2 1/4	3/4	1	8	3 1/2	19 3/8		21 1/4	22 5/8																16.0
5	48		2 1/4	3/4	1	8	3 1/2	19 3/8		21 1/4	22 5/8																16.0

STANDARD SIGN
W08-52

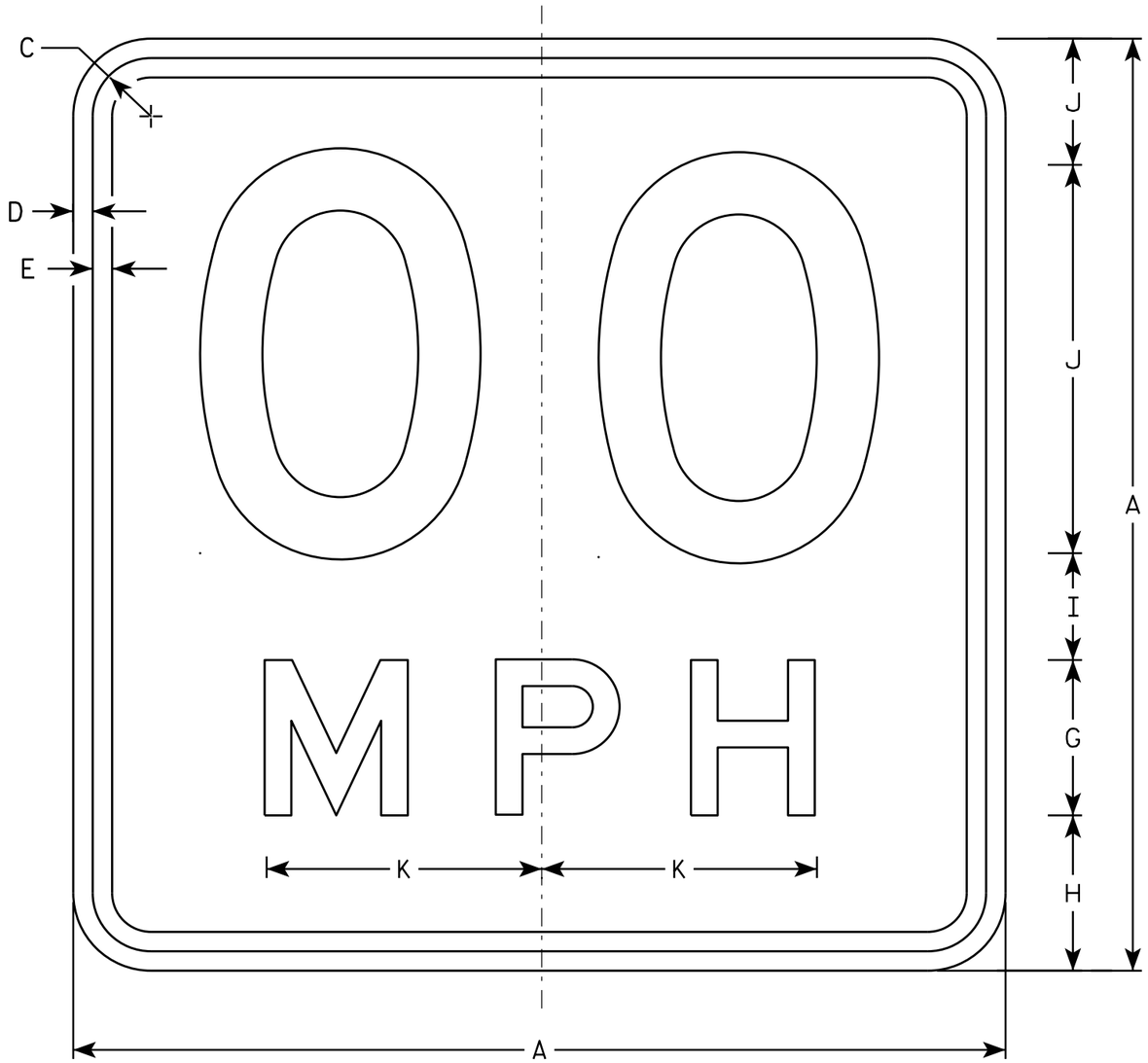
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 11/20/13 PLATE NO W08-52.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W0852.DGN PLOT DATE : 28-FEB-2014 11:41 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 6.997243:1.000000
WISDOT/CADDS SHEET 42



W013-1

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
   Line 2 is Series E

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	7 1/8																4.00
2S	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
2M	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
3	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

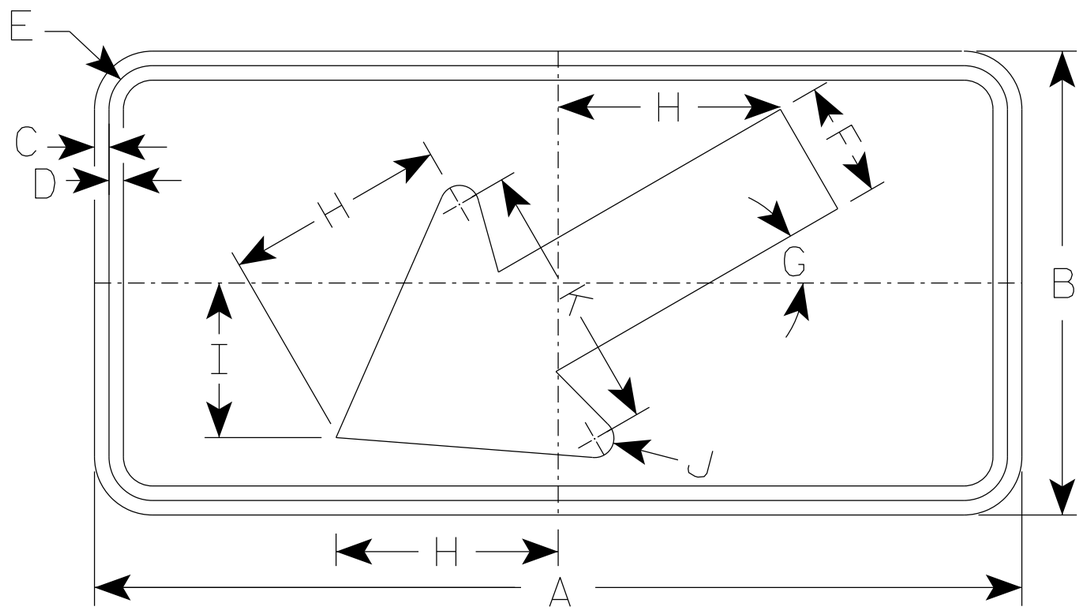
STANDARD SIGN
W013-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/21/13 PLATE 143 W013-1.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



W016-7L

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded but corners shall be rounded when base material is metal.
4. W016-7R is the same as W016-L except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
2S	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0
2M	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0
3	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0
4	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0
5	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0

STANDARD SIGN
W016-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/11/18 PLATE NO. W016-7.1

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 144 | E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W0167.dgn PLOT DATE : 11-JUL-2018 3:11 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$

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Notes

Wisconsin Department of Transportation

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through innovation and exceptional service.

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