

RHI MAR 10, 2020

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 54

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

C BERLIN, RIPON ROAD SOUTH CHURCH STREET TO CITY LIMITS LOCAL ROAD GREEN LAKE COUNTY

STATE PROJECT NUMBER
6995-09-71

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6995-09-71	WISC 2020084	1

PROJECT ID: 6995-09-71

COUNTY: GREEN LAKE COUNTY



DESIGN DESIGNATION 6995-09-01

A.A.D.T.	2020	=	3000
A.A.D.T.	2040	=	3300
D.H.V.		=	22
D.D.		=	59/41
T.		=	25.2%
DESIGN SPEED		=	40
ESALS		=	1,752,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



LAYOUT
SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 1.035

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, GREEN LAKE COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

BEGIN PROJECT
STA. 10+21
Y = 317,093.21
X = 574,030.30

END PROJECT
STA. 64+88
Y = 313,434.30
X = 578,038.26

ACCEPTED FOR
CITY OF BERLIN
Date 10-31-19 *Jodi Olson*
(Signature and Title of Official)

ORIGINAL PLANS PREPARED BY
KUNKEL ENGINEERING GROUP

DATE: 10/29/19 *Basil Orechwa*
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PREPARED BY
Surveyor: KUNKEL ENGINEERING GROUP
Designer: KUNKEL ENGINEERING GROUP
Regional Examiner: J. SCHAEFFER
Regional Supervisor: Jed Peters

APPROVED FOR THE DEPARTMENT
DATE: 10/29/19 *Will Williams*
(Signature)

E

UTILITIES CONTACTS

STREET AND STORM SEWER
CITY OF BERLIN
P.O. BOX 272
BERLIN, WI 54923
920-361-5425
CONTACT: SCOTT ZABEL

WATER & SANITARY SEWER
CITY OF BERLIN
P.O. BOX 272
BERLIN, WI 54923
920-361-5404
CONTACT: BRIAN MALNORY

TELEPHONE
CENTURYLINK
P.O. BOX 98
RANDOLPH, WI 53956
920-748-8491
CONTACT: TIM KROEZE

ELECTRIC & NATURAL GAS
ALLIANT ENERGY
880 N. WISCONSIN ST.
BERLIN, WI 54923
920-361-5625
CONTACT: MARK VILLERS

CABLE TELEVISION
CHARTER COMMUNICATIONS
W7185 STH 49, SUITE 1
WAUPUN, WI 53963
920-263-0062 OFFICE
920-304-6797 CELL
CONTACT: NICK FRASE

DEPARTMENT OF NATURAL RESOURCES
920-662-5407
JEREMIAH.SCHIEFELBEIN@WISCONSIN.GOV
CONTACT: JAY SCHIEFELBEIN

KUNKEL ENGINEERING GROUP
107 PARALLEL ST
BEAVER DAM, WI 53916
920-356-9447
BORECHWA@KUNKELENGINEERING.COM
CONTACT: BASIL ORECHWA



STANDARD ABBREVIATIONS

BCAM	BLOCK CORNER MONUMENT	MH	MANHOLE
IP	IRON PIPE	WV	WATER VALVE
RB	REBAR	AV	AUXILIARY VALVE
CP	CONTROL POINT	UP	POWER POLE/UTILITY POLE
R/W	RIGHT OF WAY	LP	LIGHT POLE
PL	PROPERTY LINE	STM	STORM SEWER
		INL	STORM INLET
EX	EXISTING	SAN	SANITARY SEWER
TBR	TO BE REMOVED	CS	CURB STOP
TBA	TO BE ABANDONED	STA	STATION
		CL	CENTERLINE
CSW	CONCRETE SIDEWALK	T/C	TOP OF CURB
DRWY	DRIVEWAY	B/W	BACK OF WALK
CONC	CONCRETE	ER	END OF RADIUS
ASPH	ASPHALT	PC	POINT OF CURVATURE
GRAV	GRAVEL	PI	POINT OF INTERSECTION
PAVT	PAVEMENT	PT	POINT OF TANGENCY
NTS	NOT TO SCALE		

GENERAL NOTES

THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

RADIUS DIMENSIONS FOR THE CURB AND GUTTER ARE TO THE BACK OF CURB, UNLESS OTHERWISE NOTED. ALL OTHER RADII ARE MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN OR NOTED ON THE PLAN.

CURB AND GUTTER PLAN GRADES ARE TO THE TOP OF CURB LINE UNLESS OTHERWISE NOTED.

EXPANSION JOINTS TO BE CONSTRUCTED AT ALL RADIUS POINTS IN CURB AND GUTTER OR AT LOCATIONS SHOWN ON THE PLAN. CONSTRUCT INSIDE EDGE OF SIDEWALK 1/2 INCH HIGHER THAN TOP OF CURB WHEN THEY ARE ADJACENT TO EACH OTHER.

SAWCUTS, AS SHOWN ON PLANS, ARE AT SUGGESTED LOCATIONS AND MAY BE ADJUSTED AT THE DISCRETION OF THE ENGINEER TO BETTER SUIT FIELD CONDITIONS.

PAVEMENT REMOVAL WILL BE TO THE NEAREST JOINT OR A SAWED EDGE WILL BE REQUIRED AS DIRECTED BY THE ENGINEER.

EXISTING DRAINAGE FACILITIES WILL REMAIN FUNCTIONAL DURING EXCAVATION OPERATIONS.

ASPHALTIC COMMERCIAL AND PRIVATE ENTRANCES SHALL BE SAWCUT AS REQUIRED TO PROVIDE A CLEAN BUTT JOINT TO NEW WORK.

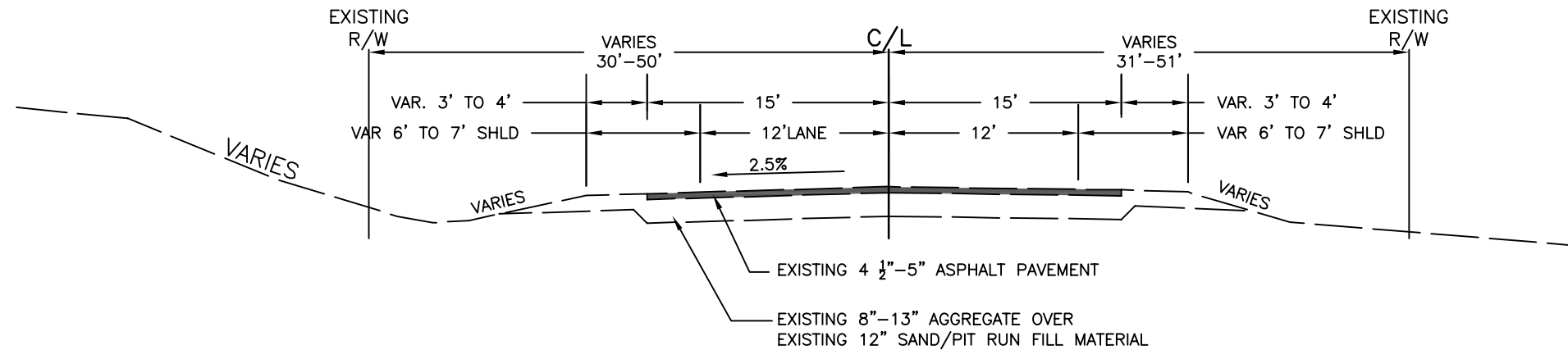
WHEREVER ASPHALTIC CONCRETE PAVEMENT ABUTS OR OVERLAPS ONTO PORTLAND CEMENT CONCRETE, THE EXISTING PAVEMENT SHALL RECEIVE A TACK COAT OF RS-1 OR RS-2 EMULSIFIED ASPHALT. APPLICATION RATE FOR TACK COAT SHALL BE 0.05 GALLONS PER SQUARE YARD.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

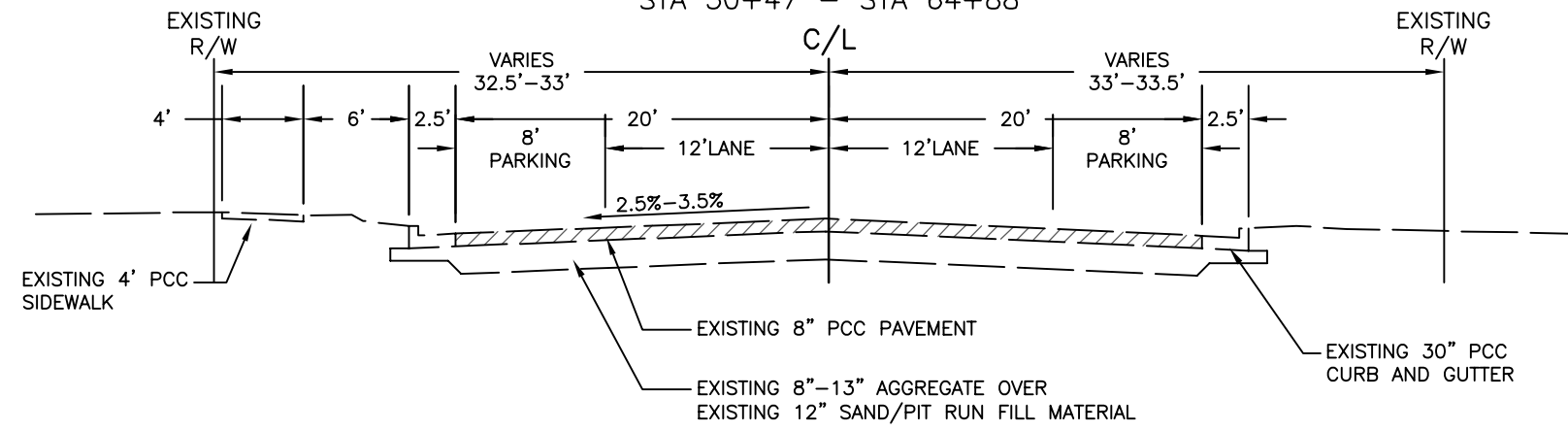
THE EROSION CONTROL DEVICES AS SHOWN ON THE PAVING PLAN SHEETS ARE AT SUGGESTED LOCATIONS. THE EXACT LOCATION WILL BE DETERMINED BY THE ENGINEER.

THE CONTRACTOR WILL COORDINATE WITH LOCAL BUSINESSES WHEN ACCESS TO THE BUSINESS WILL BE CLOSED OFF FOR SIDEWALK INSTALLATION AND ANY OTHER WORK THAT MAY IMPACT THE PEDESTRIAN ACCESS AS LAID OUT IN THE 'TRAFFIC CONTROL - PEDESTRIAN ACCESS' SECTION OF THE PLAN.

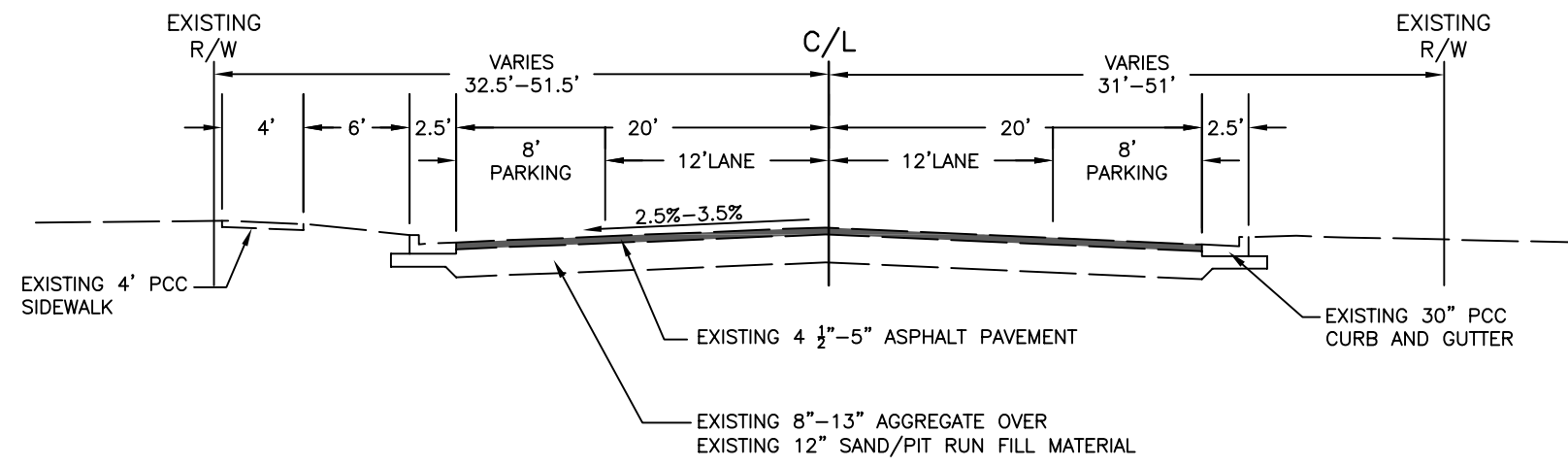
REMOVAL OF EROSION CONTROL DEVICES SHALL BE INCIDENTAL TO RESPECTIVE BID ITEMS.



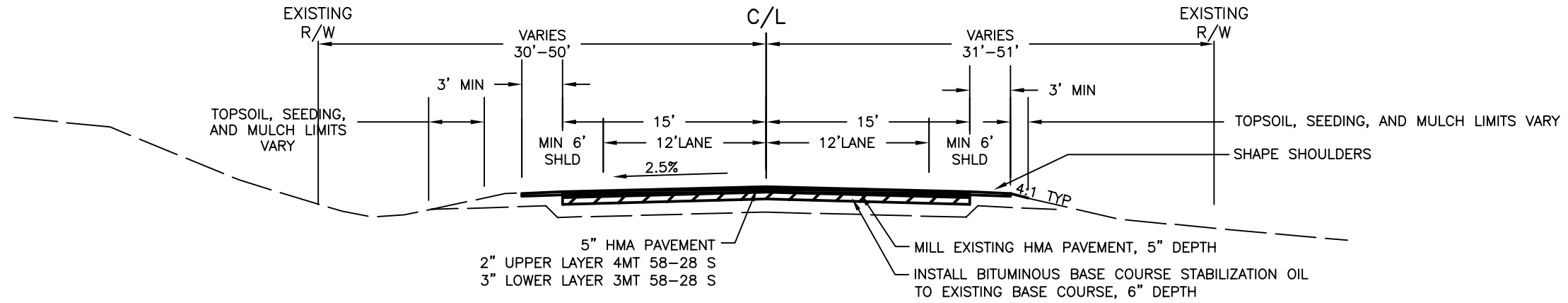
EXISTING TYPICAL SECTION
RIPON ROAD
STA 30+47 - STA 64+88



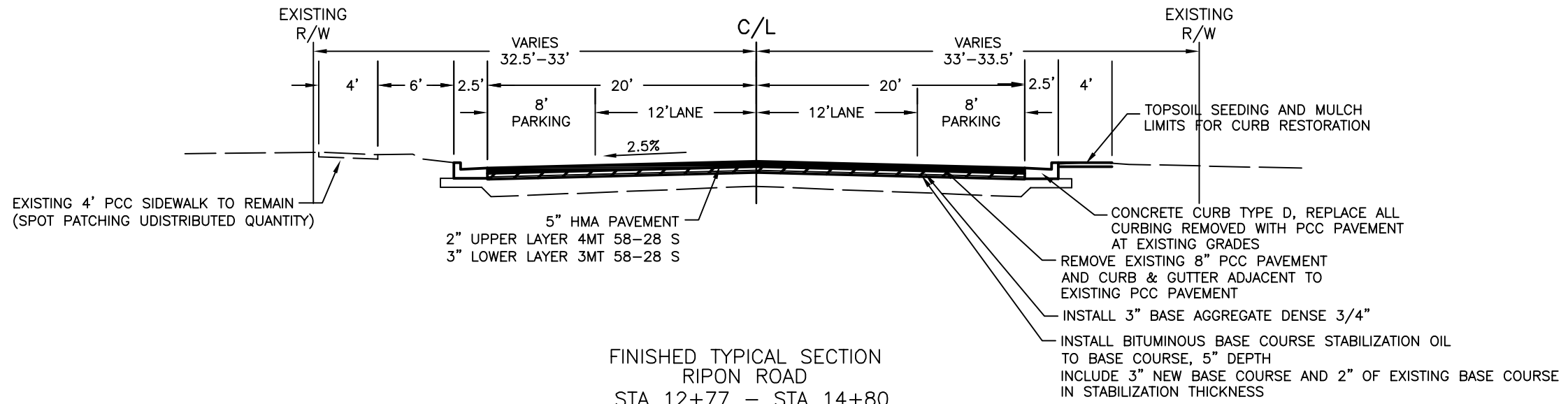
EXISTING TYPICAL SECTION
RIPON ROAD
STA 12+77 - STA 14+80



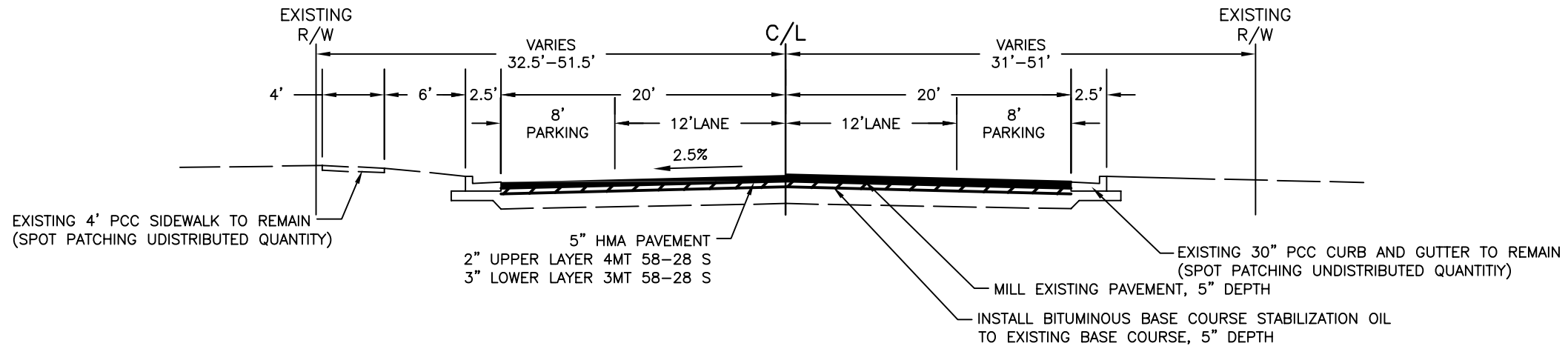
EXISTING TYPICAL SECTION
RIPON ROAD
STA 10+22 - STA 12+77
STA 14+80 - STA 30+47



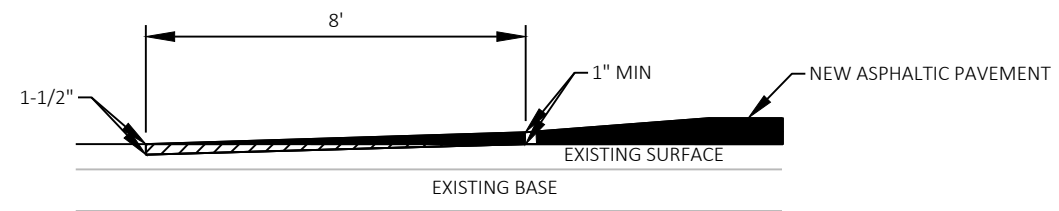
FINISHED TYPICAL SECTION
RIPON ROAD
STA 30+47 - STA 64+88



FINISHED TYPICAL SECTION
RIPON ROAD
STA 12+77 - STA 14+80

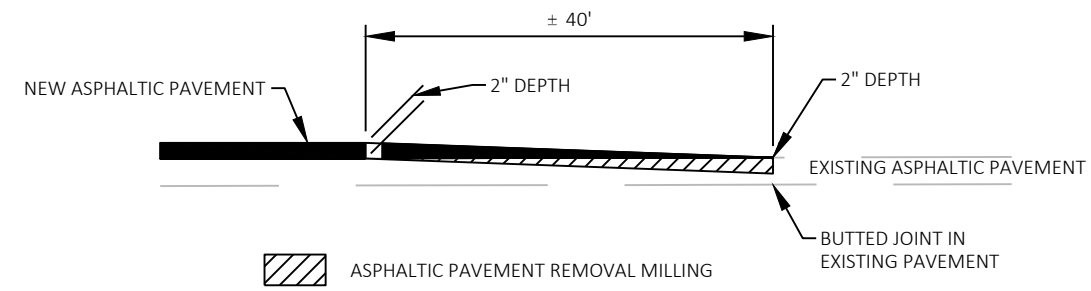


FINISHED TYPICAL SECTION
RIPON ROAD
STA 10+22 - STA 12+77
STA 14+80 - STA 30+47

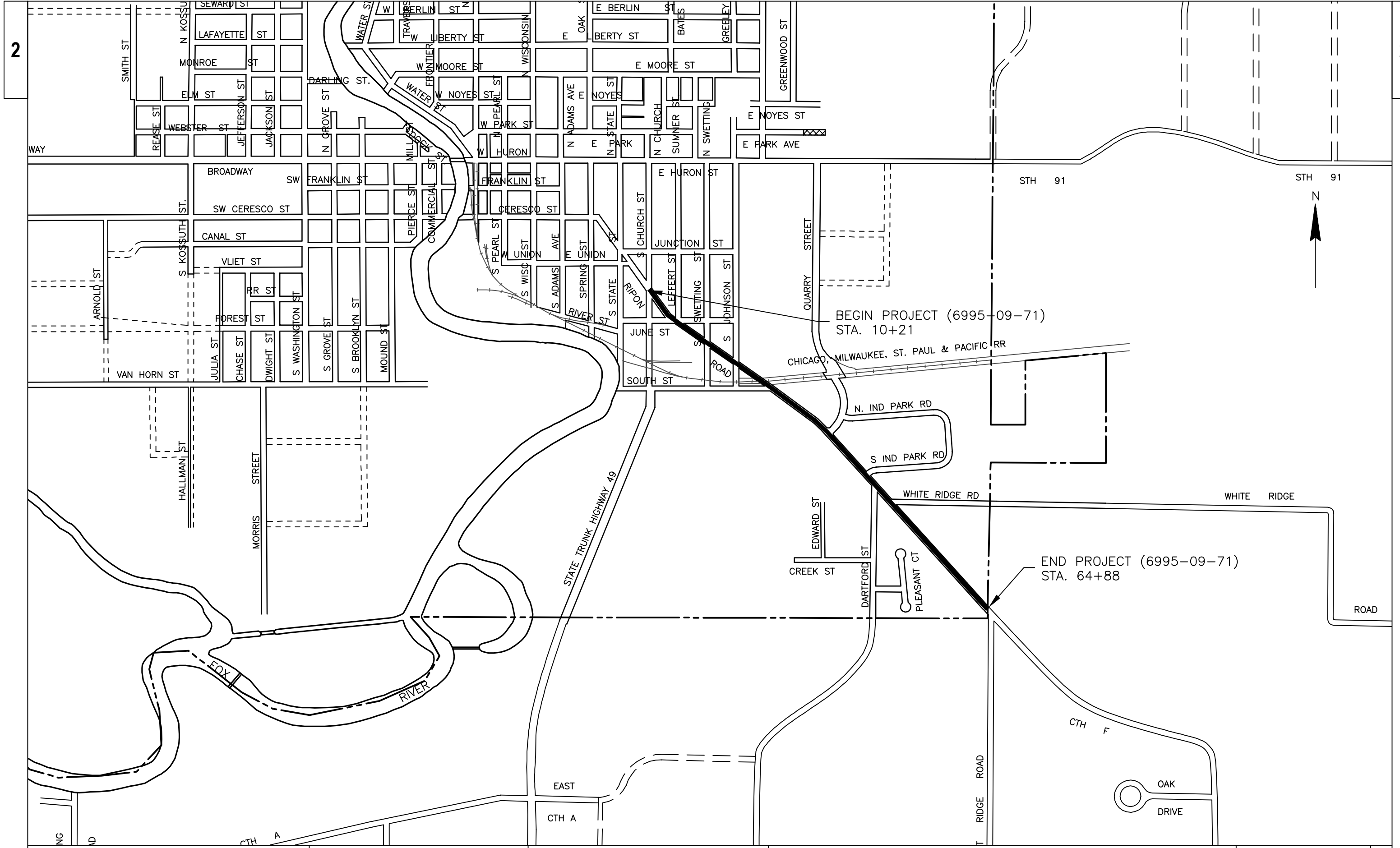


 ASPHALTIC PAVEMENT REMOVAL MILLING

SIDE ROAD BUTT JOINT



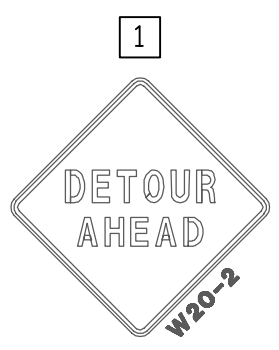
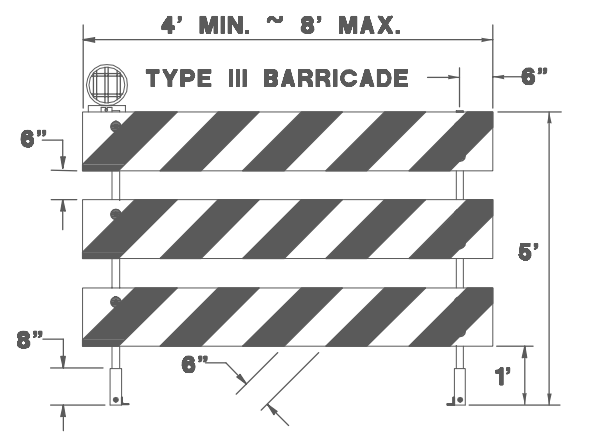
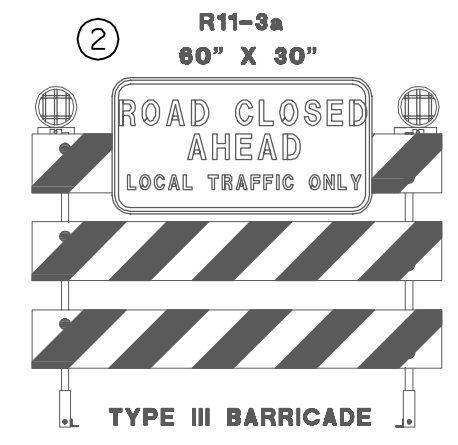
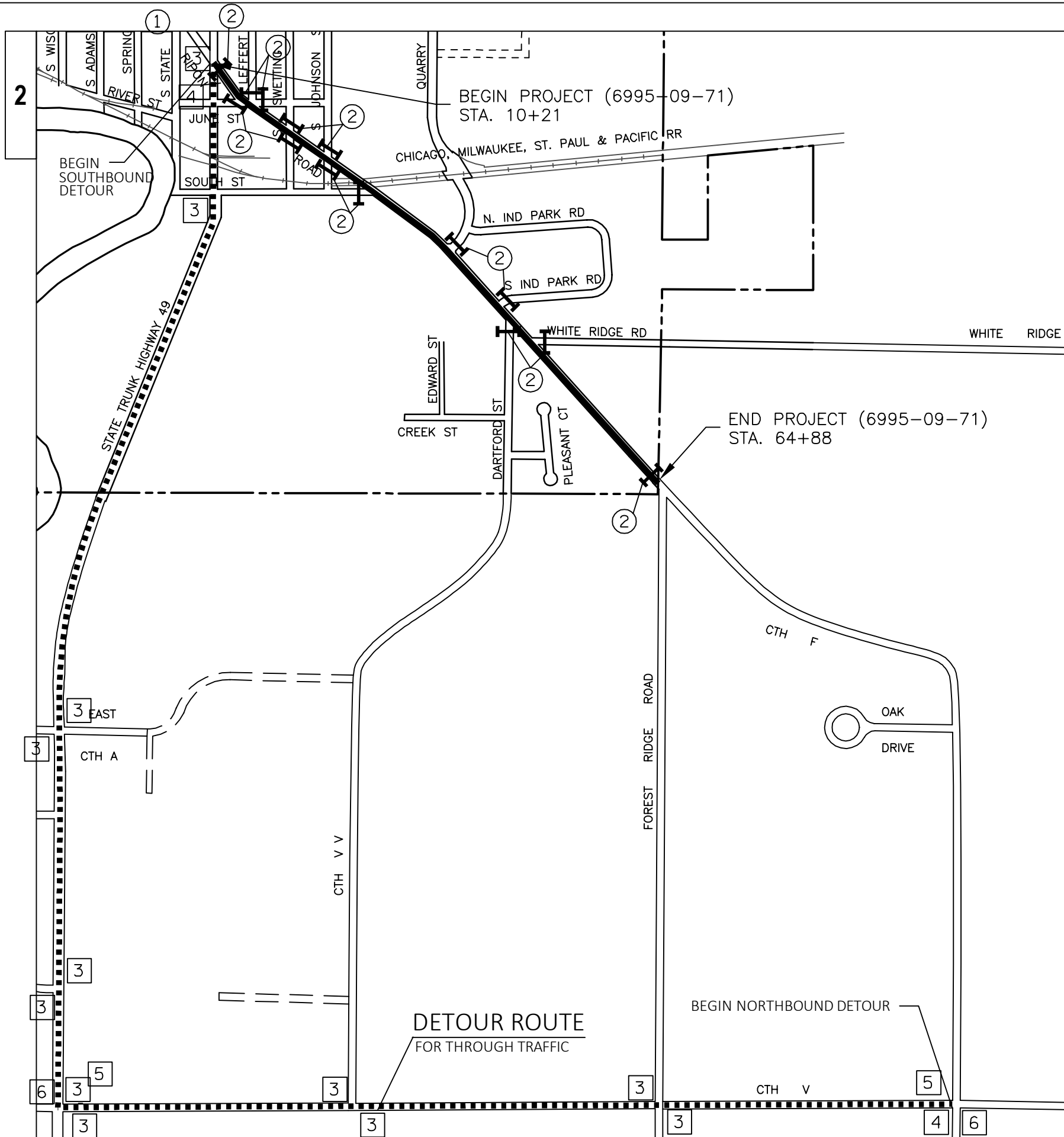
MAINLINE BUTT JOINT



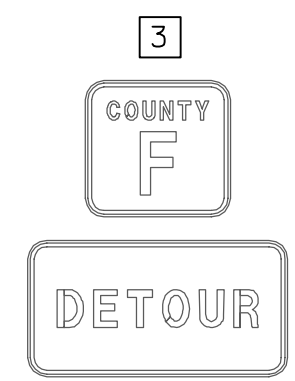
PROJECT NO: 6995-09-71	HWY: RIPON ROAD	COUNTY: GREEN LAKE	PROJECT OVERVIEW	SHEET E
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FILE NAME : M:\TOWNS\BERLIN_CITY\RIPON ROAD IMPROVEMENTS\DWG\02-OVERVIEW MAP.DWG PLOT DATE : 8/21/2019 2:44 PM PLOT BY : ALEX WILLIAMS PLOT NAME : PLOT SCALE : 1"=1000' WISDOT/CADD SHEET 42

LAYOUT NAME - 02-PROJECT OVERVIEW

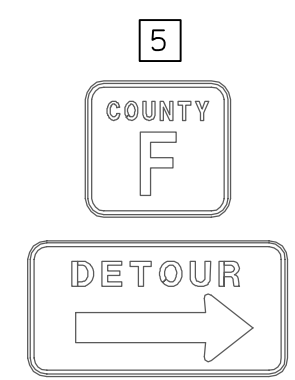


LOCATE 500'
AHEAD OF
DETOUR

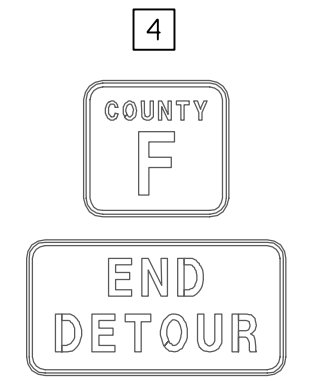


M4-8
24" X 12"

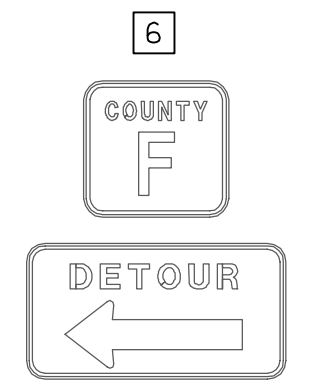
PROVIDE DETOUR
ROUTE SIGNAGE
PER SDD 15C02-07a



M4-9R
30" X 24"



M4-8a
24" X 18"



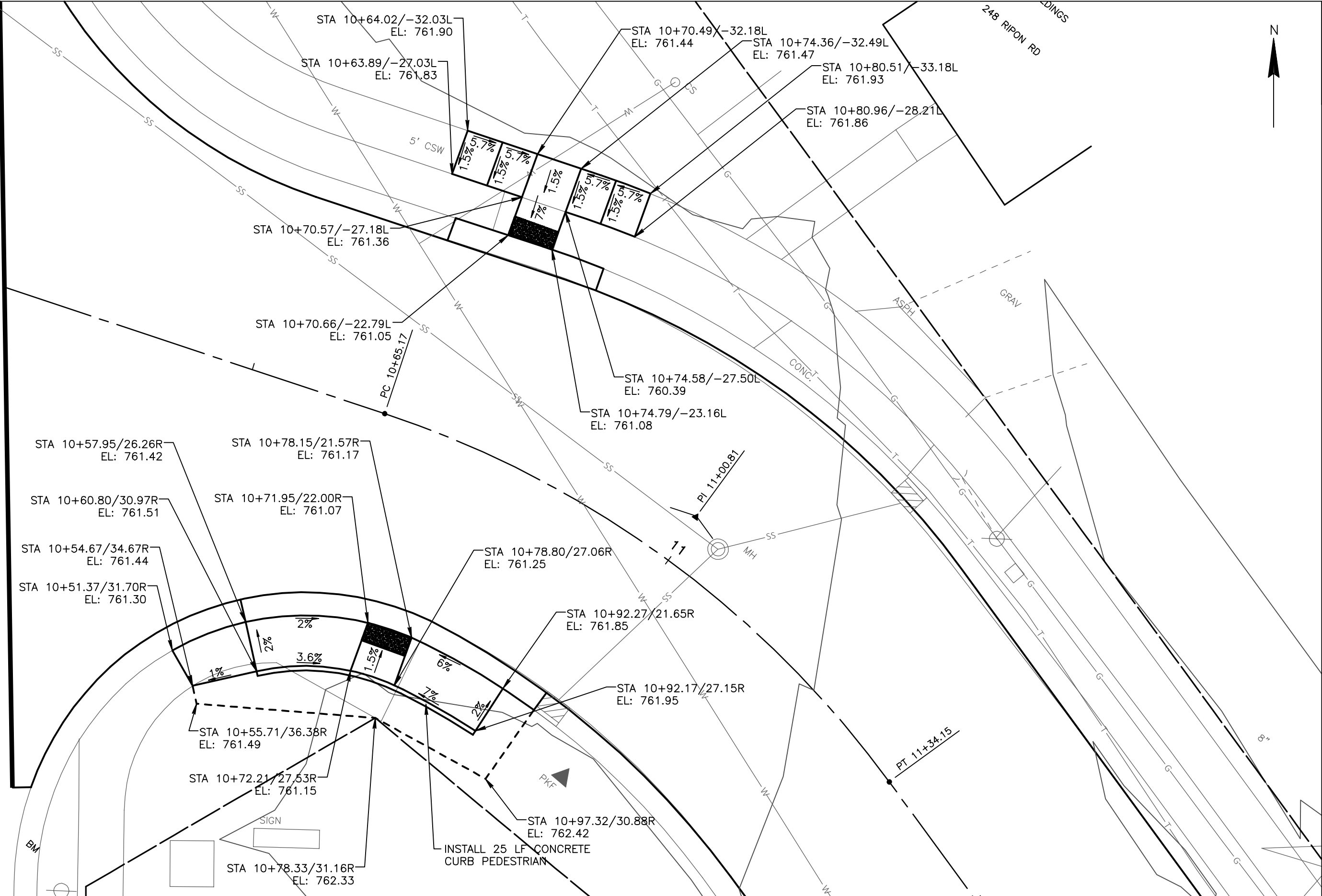
M4-9L
30" X 24"

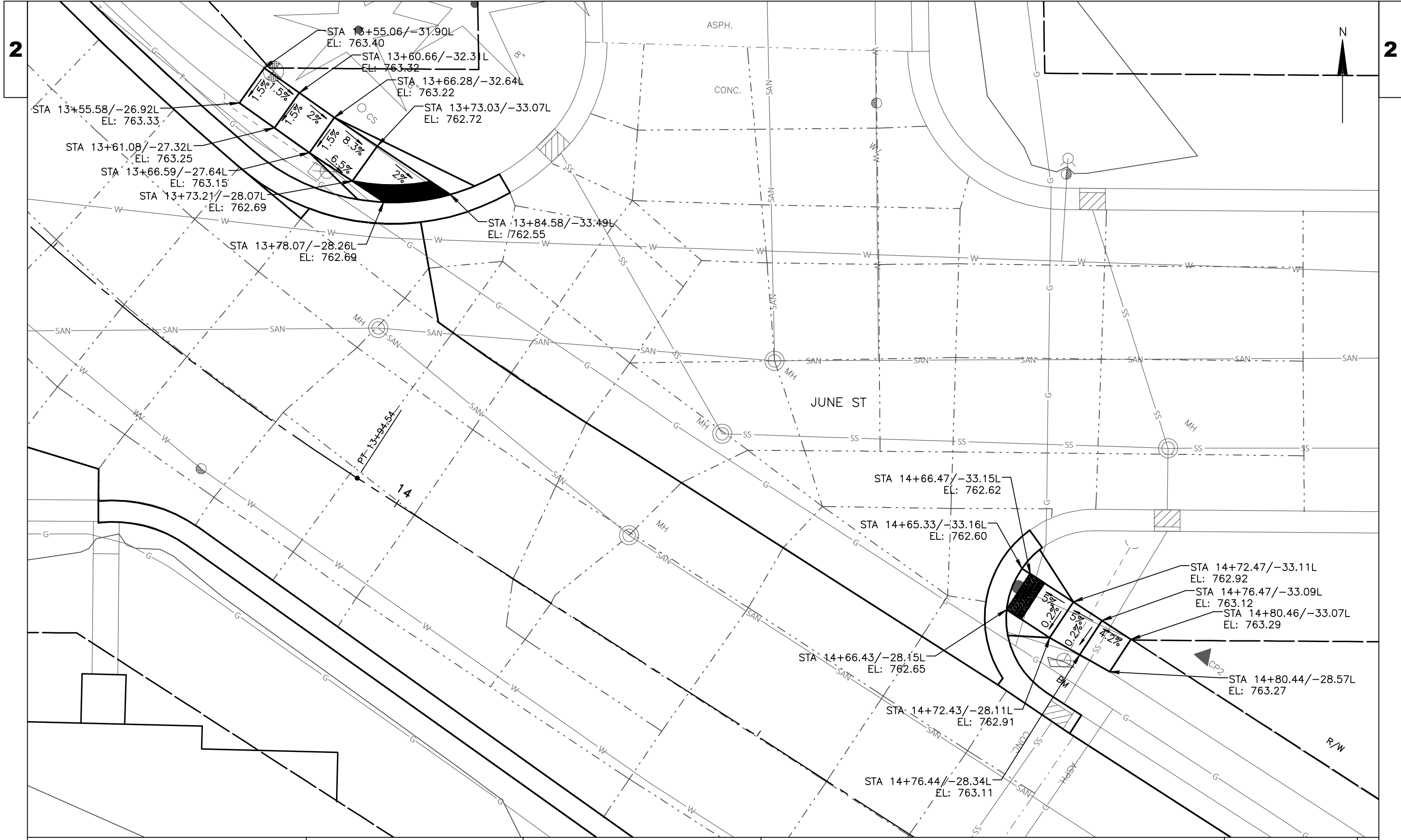
GENERAL NOTES

COVER OR REMOVE CURRENT SIGN MESSAGES CONFLICTING WITH TRAFFIC CONTROL

ACCESS TO ALL EXISTING PROPERTIES WITHIN THE PROJECT LIMITS MUST BE MAINTAINED THROUGHOUT CONSTRUCTION, DURING TIMES WHEN CONSTRUCTION DOES NOT ALLOW ACCESS, PROPERTY OWNERS SHALL BE NOTIFIED WELL IN ADVANCE BY THE CONTRACTOR. ALL CROSS STREET WILL BE CLOSED USING TYPE III BARRICADES. PLACE DESIGNATED SIGNS ON TYPE III BARRICADES IN THE PATH OF APPROACHING TRAFFIC.

- # SIGN ON PERMANENT SUPPORT
- ② TYPE III BARRICADE WITH TYPE 'A' FLASHERS. (TWO EACH BARRICADE) WARNING SIGN OR SIGNS TO BE MOUNTED ON BARRICADE WHERE INDICATED ON PLANS.





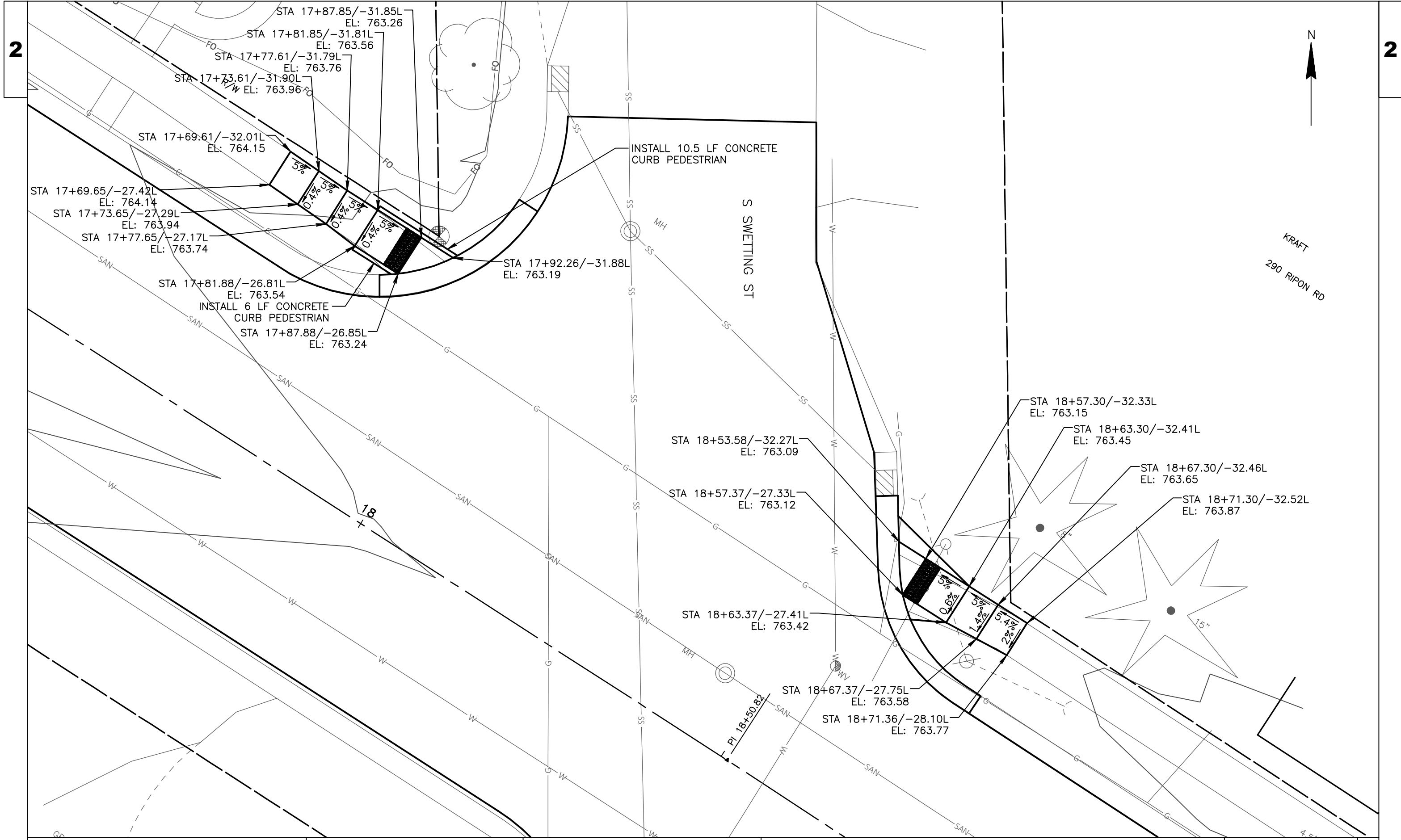
2

2

PROJECT NO: 6995-09-71	HWY: CITY OF BERLIN, RIPON RD	COUNTY: GREEN LAKE	CURB RAMP DETAIL	SHEET	E
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FILE NAME : M:\TOWNS\BERLIN_CITY\RIPON ROAD IMPROVEMENTS\DWG\05-RIPON_RD-6.DWG PLOT DATE : 8/21/2019 2:44 PM PLOT BY : ALEX WILLIAMS PLOT NAME : PLOT SCALE : 1 IN:10 FT WISDOT/CADS SHEET 42

LAYOUT NAME - Curb Ramps (2)



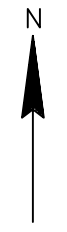
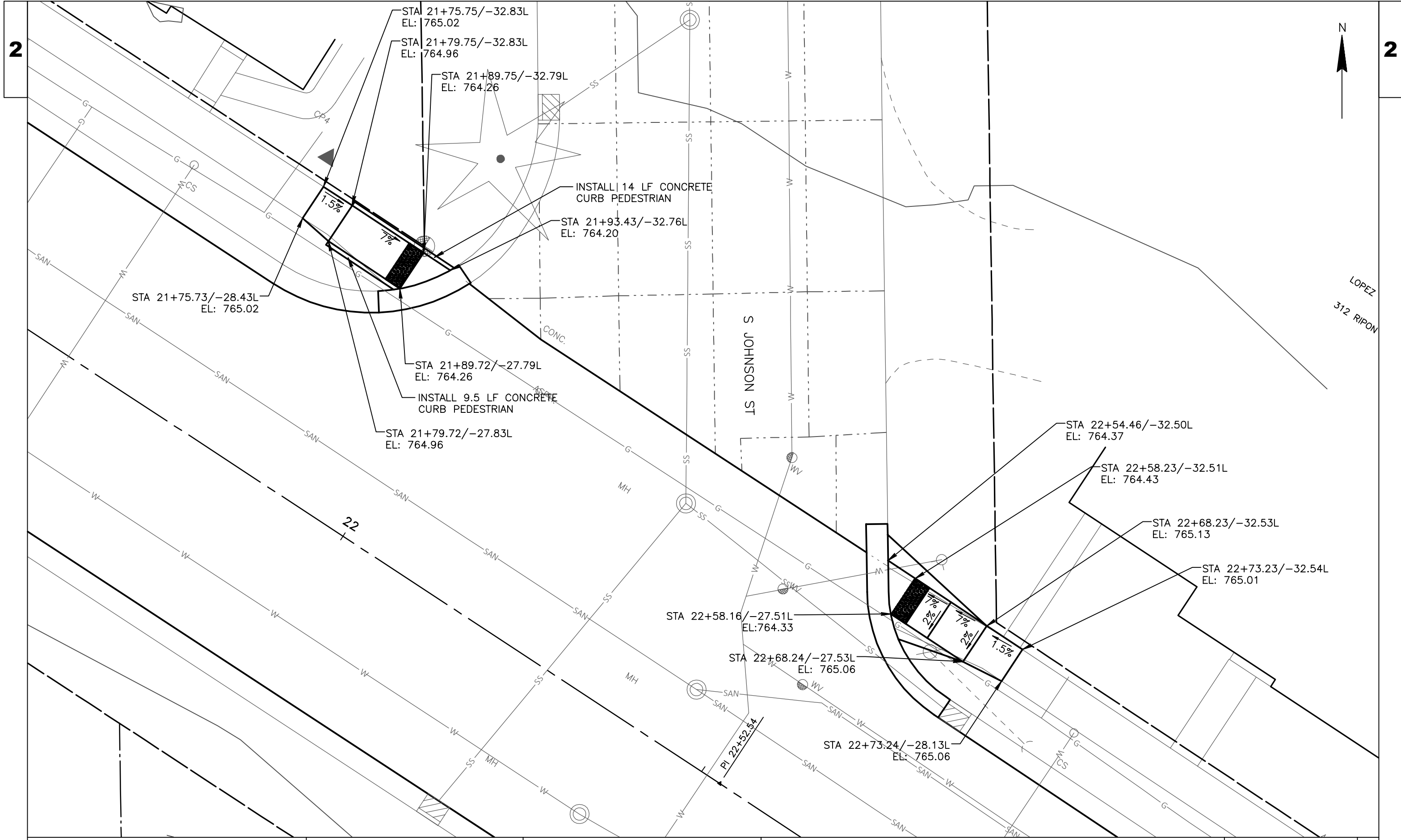
PROJECT NO: 6995-09-71

HWY: CITY OF BERLIN, RIPON RD COUNTY: GREEN LAKE

CURB RAMP DETAIL

SHEET

E



2

2

Estimate Of Quantities

6995-09-71

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Pavement	SY	880.000	880.000
0004	204.0110	Removing Asphaltic Surface	SY	180.000	180.000
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	509.000	509.000
0008	204.0120	Removing Asphaltic Surface Milling	SY	22,427.000	22,427.000
0010	204.0150	Removing Curb & Gutter	LF	499.000	499.000
0012	204.0155	Removing Concrete Sidewalk	SY	90.000	90.000
0014	205.0100	Excavation Common	CY	2,243.000	2,243.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	183.000	183.000
0018	305.0500	Shaping Shoulders	STA	68.800	68.800
0020	312.0115	Select Crushed Material	CY	2,243.000	2,243.000
0022	455.0605	Tack Coat	GAL	1,714.000	1,714.000
0024	460.2000	Incentive Density HMA Pavement	DOL	4,740.000	4,740.000
0026	460.6223	HMA Pavement 3 MT 58-28 S	TON	4,408.000	4,408.000
0028	460.6224	HMA Pavement 4 MT 58-28 S	TON	2,939.000	2,939.000
0030	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	45.000	45.000
0032	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	726.000	726.000
0034	601.0600	Concrete Curb Pedestrian	LF	65.000	65.000
0036	602.0405	Concrete Sidewalk 4-Inch	SF	941.000	941.000
0038	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	70.000	70.000
0040	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	17.000	17.000
0042	619.1000	Mobilization	EACH	1.000	1.000
0044	625.0100	Topsoil	SY	326.000	326.000
0046	627.0200	Mulching	SY	326.000	326.000
0048	628.1504	Silt Fence	LF	400.000	400.000
0050	628.7015	Inlet Protection Type C	EACH	14.000	14.000
0052	630.0140	Seeding Mixture No. 40	LB	9.000	9.000
0054	630.0500	Seed Water	MGAL	7.000	7.000
0056	642.5001	Field Office Type B	EACH	1.000	1.000
0058	643.5000	Traffic Control	EACH	1.000	1.000
0060	646.1020	Marking Line Epoxy 4-Inch	LF	13,957.000	13,957.000
0062	646.6105	Marking Stop Line Paint 18-Inch	LF	20.000	20.000
0064	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	40.000	40.000
0066	650.8000	Construction Staking Resurfacing Reference	LF	5,466.000	5,466.000
0068	SPV.0060	Special 01. Replace Manhole Casting	EACH	21.000	21.000
0070	SPV.0060	Special 02. Replace Valve Box	EACH	8.000	8.000
0072	SPV.0180	Special 01. Base Course Stabilization, Installation	SY	23,301.000	23,301.000
0074	SPV.0195	Special 01. Base Course Stabilization Oil	TON	254.000	254.000

REMOVING PAVEMENT

			204.0100
STATION	- STATION	LOCATION	SY
12+77	- 14+81	MAINLINE	880
TOTALS			880

REMOVE ASPHALTIC SUFRACE BUTT JOINTS

			204.0115
STATION	- STATION	LOCATION	SY
10+21.6		LEFT	107
18+48.5		LEFT	32
18+48.5		RIGHT	39
22+52.6		LEFT	62
26+98.1		RIGHT	41
36+76.8		LEFT	50
42+71.8		LEFT	32
45+85.8		RIGHT	23
46+80		LEFT	41
48+42.		LEFT	28
61+21		RIGHT	53
TOTALS			509

REMOVE ASPHALTIC SURFACE MILLING

			204.0120
STATION	- STATION	LOCATION	SY
10+21.6	- 12+77	MAINLINE	1217
14+81.	- 30+54	MAINLINE	7700
30+54	- 64+87.5	MAINLINE	13510
TOTALS			22427

EXCAVATION COMMON

			205.0100
STATION	- STATION	LOCATION	CY
10+21.6	- 67+87.5	EBS	2243
TOTALS			2243

REMOVING CONCRETE SIDEWALK

			204.0155
STATION	- STATION	LOCATION	SY
10+73		LEFT	15
10+73		RIGHT	18
13+77		LEFT	13
14+69		LEFT	7
17+87		LEFT	11
18+59		LEFT	8
21+87		LEFT	9
22+60		LEFT	9
TOTALS			90

REMOVING CURB AND GUTTER

			204.0150
STATION	- STATION	LOCATION	LF
10+58.	- 10+97.3	RIGHT	34
10+65.7	- 10+79.7	LEFT	17
17+86.1	- 17+95.	LEFT	20
18+50.8	- 18+71.4	LEFT	20
21+87.8	- 21+93.9	LEFT	10
22+52.3	- 22+69.5	LEFT	23
10+21.6	- 67+87.5	UNDISTRIBUTED	375
TOTALS			499

REMOVE ASPHALTIC SURFACE

			204.0110
STATION	- STATION	LOCATION	SY
10+73		RIGHT	17
34+25		LEFT	25
46+80		LEFT	110
61+21		RIGHT	28
TOTALS			180

BASE AGGREGATE DENSE

			305.0110
			3/4"
STATION	- STATION	LOCATION	TON
12+77	- 14+81.	MAINLINE	109
31+24		LEFT	26
32+60		LEFT	25
39+65		RIGHT	6
54+60		RIGHT	3
56+00		RIGHT	2
60+25		LEFT	4
61+25		LEFT	3
61+25		RIGHT	5
TOTALS			183

SHAPING SHOULDERS

			305.0500
STATION	- STATION	LOCATION	STA
30+54	- 64+87.5	LT MAINLINE	34.4
30+54	- 64+87.5	RT MAINLINE	34.4
TOTALS			68.8

SELECT CRUSHED MATERIAL

			312.0115
STATION	- STATION	LOCATION	CY
10+21.6	- 67+87.5	EBS	2243
TOTALS			2243

ASPHALTIC ITEMS

STATION	-	STATION	LOCATION	455.0605 TACK COAT GAL	460.6223 HMA PAVEMENT 3 MT 58-28 S TON	460.6224 HMA PAVEMENT 4 MT 58-28 S TON	465.0120 ASPHALTIC DRIVEWAYS AND FIELD ENTRANCES TON
10+21.6	-	30+54.	MAINLINE	721	1854	1236	
30+54	-	64+87.5	MAINLINE	993	2554	1703	
10+73			RIGHT				4
34+25			LEFT				6
46+80			LEFT				28
61+21			RIGHT				7
TOTALS				1714	4408	2939	45

CONCRETE CURB AND GUTTER, 30 INCH, TYPE D

STATION	-	STATION	LOCATION	601.0411 LF
10+58.	-	10+97.3	RIGHT	34
10+65.7	-	10+79.7	LEFT	17
12+76.7	-	13+81.5	LEFT	100
14+71.4	-	14+80.4	LEFT	10
12+77.7	-	12+94.	RIGHT	17
13+73.6	-	14+70.2	RIGHT	100
17+86.1	-	17+95.	LEFT	20
18+50.8	-	18+71.4	LEFT	20
21+87.8	-	21+93.9	LEFT	10
22+52.3	-	22+69.5	LEFT	23
10+21.6	-	67+87.5	UNDISTRIBUTED	375
TOTALS				726

CONCRETE CURB PEDESTRIAN

STATION	-	STATION	LOCATION	601.0600 LF
10+73			RIGHT	25
17+87			LEFT	16.5
21+87			LEFT	23.5
TOTALS				65

CONCRETE SIDEWALK 4-INCH

STATION	-	STATION	LOCATION	602.0405 SF
10+73			LEFT	127
10+73			RIGHT	192
13+77			LEFT	145
14+69			LEFT	90
17+87			LEFT	100
18+59			LEFT	96
21+87			LEFT	80
22+60			LEFT	111
TOTALS				941

CURB RAMP DETECTABLE WARNING FIELD YELLOW

STATION	-	STATION	LOCATION	602.0505 SF
10+73			LEFT	10
10+73			RIGHT	10
14+69			LEFT	10
17+87			LEFT	10
18+59			LEFT	10
21+87			LEFT	10
22+60			LEFT	10
TOTALS				70

CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW

STATION	-	STATION	LOCATION	602.0605 SF
13+77			LEFT	17
TOTALS				17

FIELD OFFICE TYPE B

LOCATION	642.5001 EA
RIPON ROAD	1
TOTALS	1

MOBILIZATION

LOCATION	619.1000 EA
RIPON ROAD	1
TOTALS	1

TOPSOIL

STATION	-	STATION	LOCATION	625.0100 SY
12+76.7	-	13+81.5	LEFT	45
14+71.4	-	14+80.4	LEFT	5
12+77.7	-	12+94.	RIGHT	8
13+73.6	-	14+70.2	RIGHT	45
10+21.6	-	67+87.5	UNDISTRIBUTED	223
TOTALS				326

MULCHING

STATION	-	STATION	LOCATION	627.0200 SY
12+76.7	-	13+81.5	LEFT	45
14+71.4	-	14+80.4	LEFT	5
12+77.7	-	12+94.	RIGHT	8
13+73.6	-	14+70.2	RIGHT	45
10+21.6	-	67+87.5	UNDISTRIBUTED	223
TOTALS				326

PAVEMENT MARKING

STATION	-	STATION	LOCATION	646.1005 EDGE LINE LF	646.1005 CENTERLINE (DASHED) LF	646.1005 CENTERLINE (DOUBLE) LF	646.6105 STOP LINE 18" LF	646.7420 CROSS WALK 6" LF
10+50			LEFT				20	
10+75			MAINLINE					40
10+50	-	11+35	MAINLINE			170		
11+35	-	30+55	MAINLINE		480			
30+55	-	64+88	MAINLINE			6866		
30+55	-	36+10	LEFT	555				
37+50	-	42+60	LEFT	510				
43+90	-	48+05	LEFT	415				
49+35	-	64+88	LEFT	1553				
30+55	-	45+25	RIGHT	1470				
45+50	-	64+88	RIGHT	1938				
TOTALS				6441	480	7036	20	40

CONSTRUCTION STAKING RESURFACING REFERENCE

STATION	-	STATION	LOCATION	650.8000 LF
10+21.6	-	64+87.5	MAINLINE	5466
TOTALS				5466

SEEDING MIXTURE NO. 40

STATION	-	STATION	LOCATION	630.0140 LB
12+76.7	-	13+81.5	LEFT	1
14+71.4	-	14+80.4	LEFT	1
12+77.7	-	12+94.	RIGHT	1
13+73.6	-	14+70.2	RIGHT	1
10+21.6	-	67+87.5	UNDISTRIBUTED	5
TOTALS				9

SEED WATER

STATION	-	STATION	LOCATION	630.0500 MGAL
10+21.6	-	67+87.5	UNDISTRIBUTED	7
TOTALS				7

SILT FENCE

STATION	-	STATION	LOCATION	628.1504 EA
40+00	-	44+00	RIGHT	400
TOTALS				400

INLET PROTECTION TYPE C

STATION	LOCATION	628.7015 EA
10+99	RIGHT	1
11+14	LEFT	1
13+91	LEFT	1
14+77	LEFT	1
14+77	RIGHT	1
14+78	LEFT	1
1790.1	LEFT	1
18+50	LEFT	1
19+10	RIGHT	1
21+92	LEFT	1
22+26	RIGHT	1
22+71	LEFT	1
29+51	RIGHT	1
29+51	LEFT	1
TOTALS		14

TRAFFIC CONTROL

LOCATION	643.5000 EA
RIPON ROAD	1
TOTALS	1

REPLACE MANHOLE CASTING

SPV.0060.01

Station	LOCATION	EA
11+03.5	LEFT	1
12+49.2	RIGHT	1
13+86.7	LEFT	1
14+24.8	LEFT	1
16+17.6	LEFT	1
18+07.9	LEFT	1
18+45.1	LEFT	1
18+48.7	RIGHT	1
18+62.2	RIGHT	1
22+31.3	LEFT	1
22+40.8	RIGHT	1
22+44.3	LEFT	1
22+58.8	RIGHT	1
25+59.1	LEFT	1
26+35.5	RIGHT	1
26+99.6	RIGHT	1
27+18.8	RIGHT	1
36+62.3	LEFT	1
36+69.6	LEFT	1
36+86.5	RIGHT	1
43+19.3	LEFT	1
TOTALS		21

REPLACE WATER VALVE BOX

SPV.0060.02

STATION - STATION	LOCATION	EA
13+33.2	RIGHT	1
13+79.2	RIGHT	1
18+55.4	LEFT	1
22+46.2	LEFT	1
22+54.2	LEFT	1
36+92.2	LEFT	1
42+87.4	LEFT	1
42+98.5	LEFT	1
TOTALS		8

BASE COURSE STABILIZATION, INSTALLATION

SPV.0180.01

STATION - STATION	LOCATION	SY
10+21.6 - 12+77	MAINLINE	1217
12+77. - 14+81	MAINLINE	874
14+81. - 30+54	MAINLINE	7700
30+54 - 64+87.5	MAINLINE	13510
TOTALS		23301

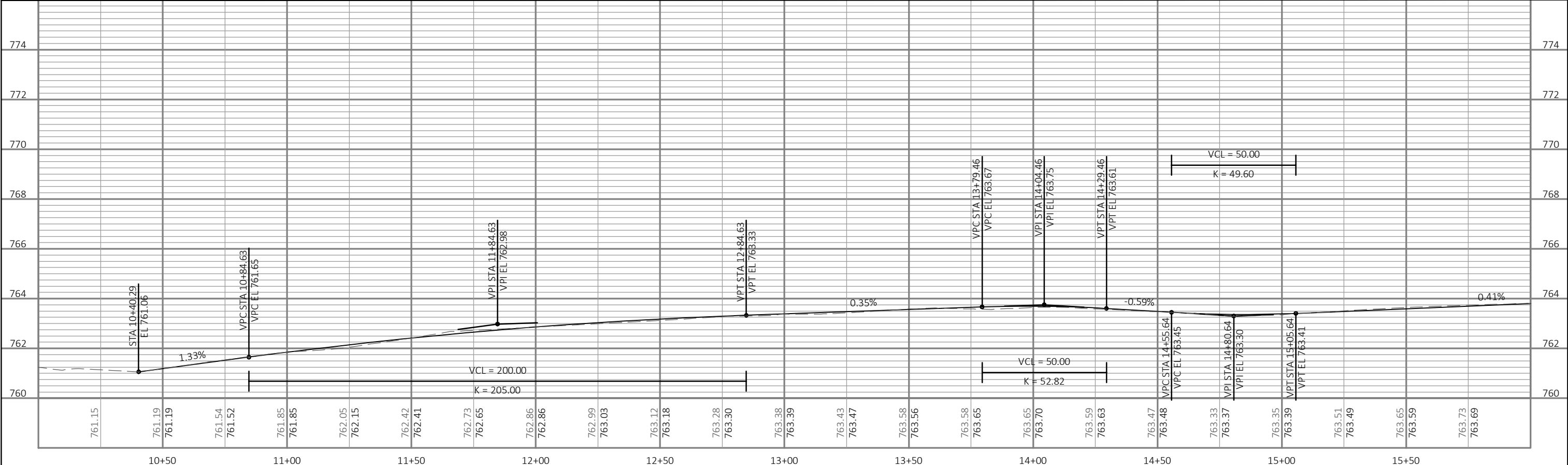
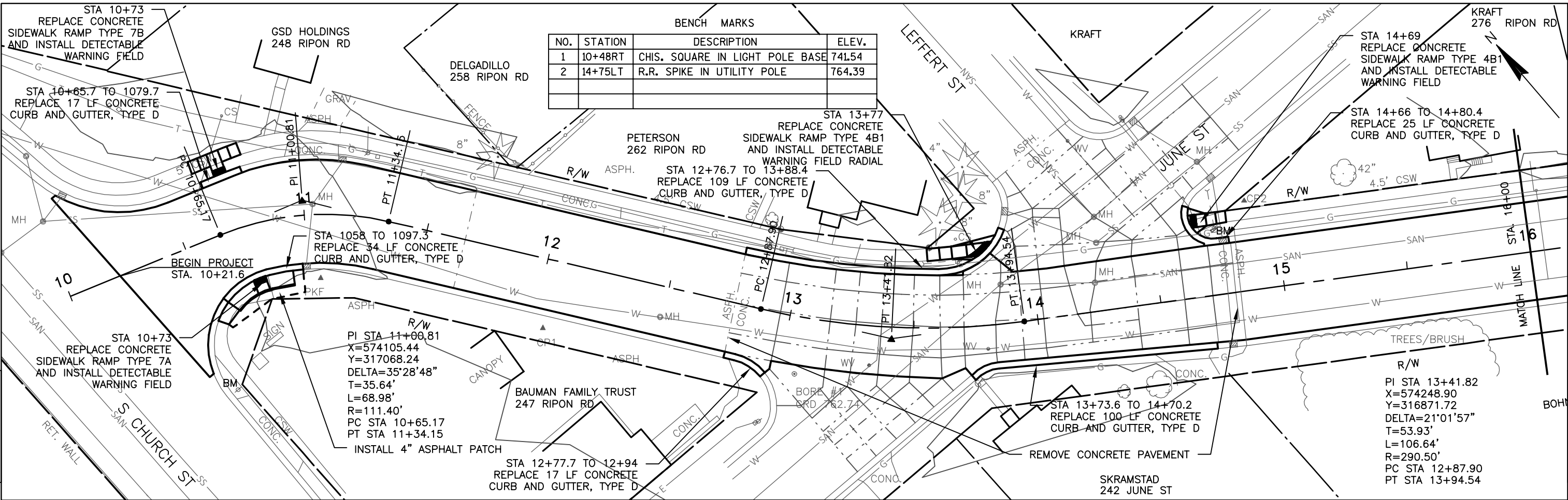
BASE COURSE STABILIZATION OIL

SPV.0195.01

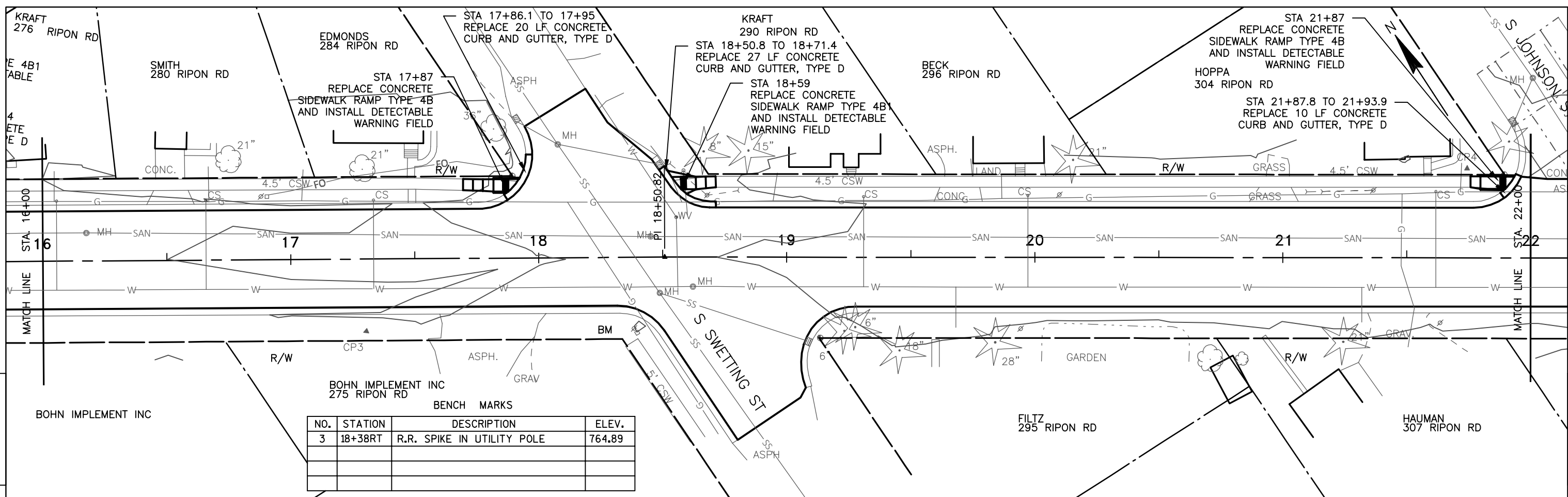
STATION - STATION	LOCATION	TON
10+21.6 - 12+77	MAINLINE	12
12+77. - 14+81	MAINLINE	9
14+81. - 30+54	MAINLINE	75
30+54 - 64+87.5	MAINLINE	158
TOTALS		254

3

3



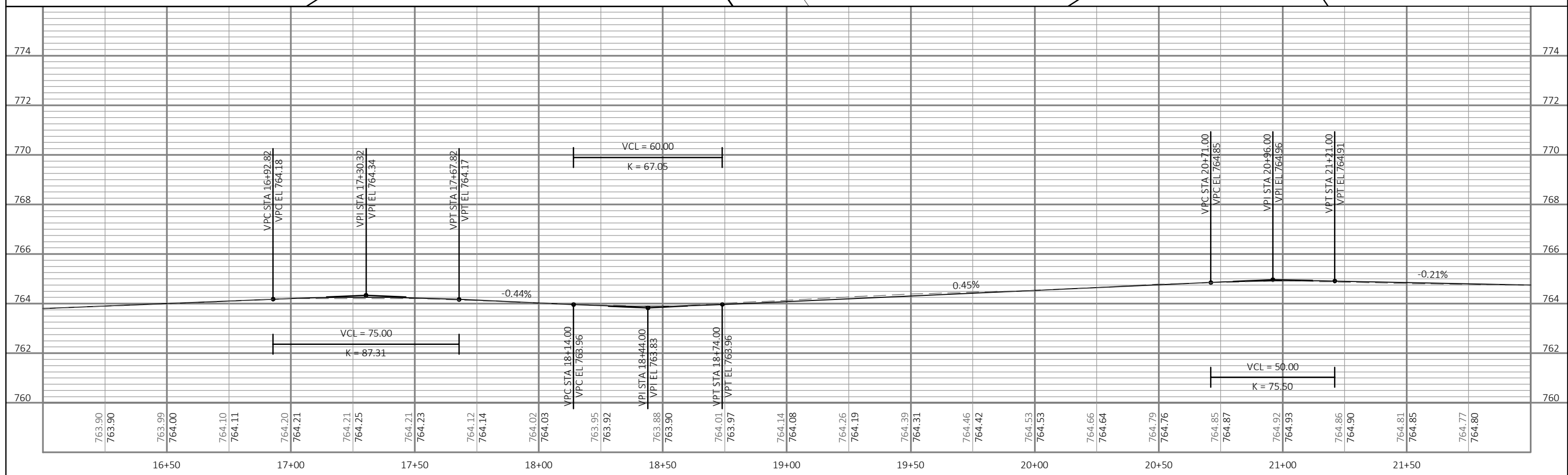
PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE PLAN AND PROFILE: RIPON ROAD RESURFACING SHEET: E



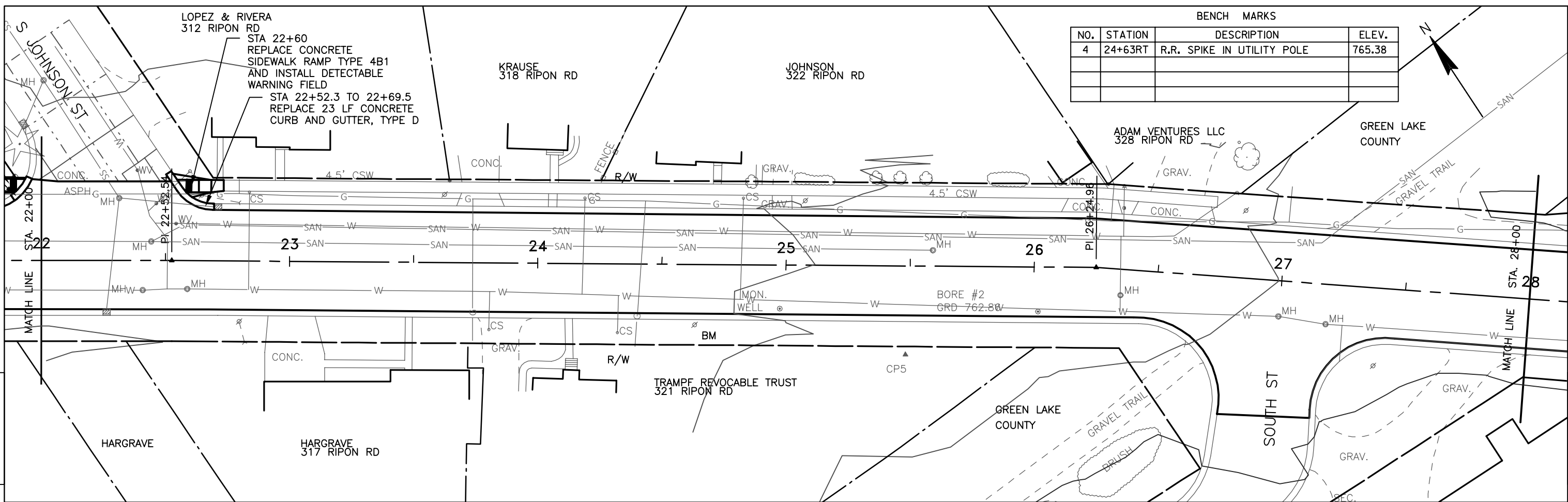
BOHN IMPLEMENT INC
275 RIPON RD

BENCH MARKS

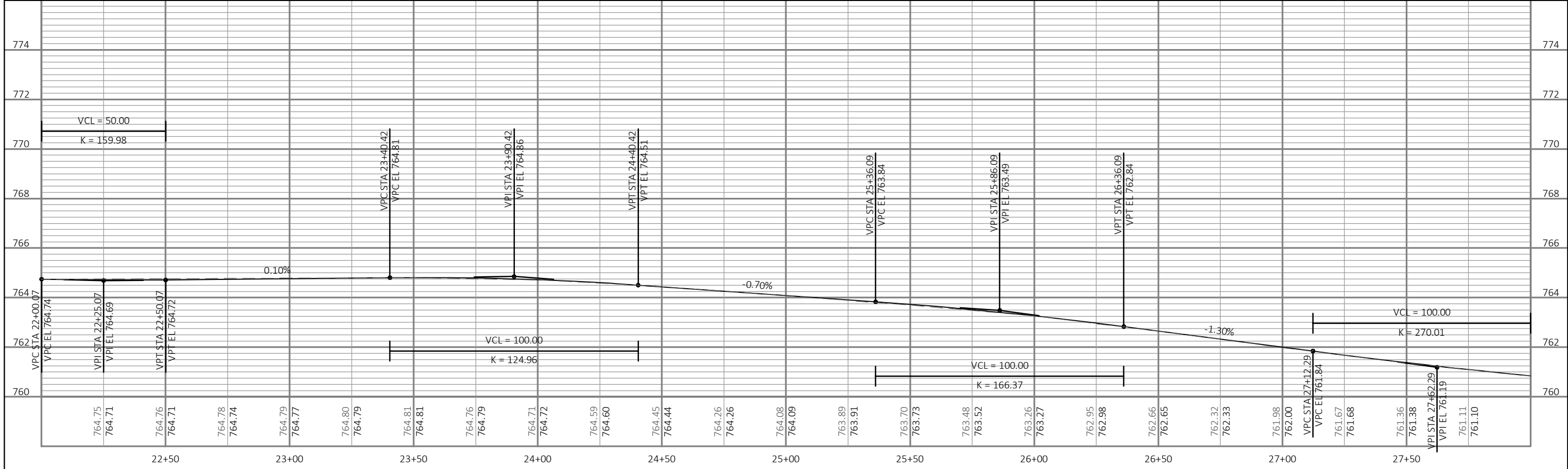
NO.	STATION	DESCRIPTION	ELEV.
3	18+38RT	R.R. SPIKE IN UTILITY POLE	764.89



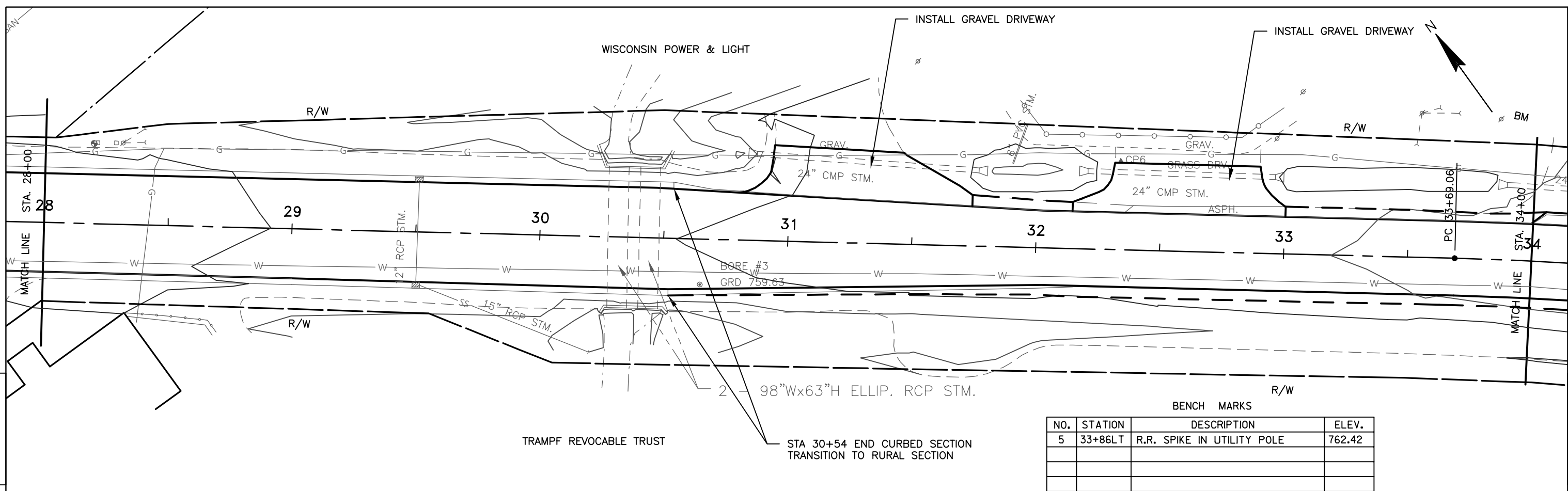
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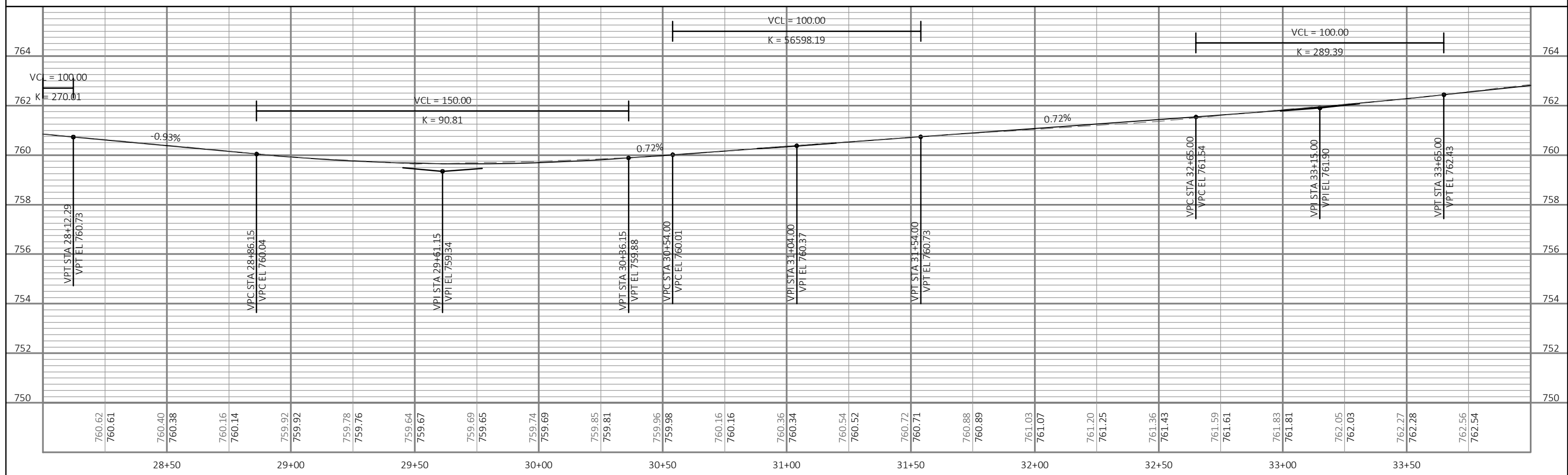
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
4	24+63RT	R.R. SPIKE IN UTILITY POLE	765.38



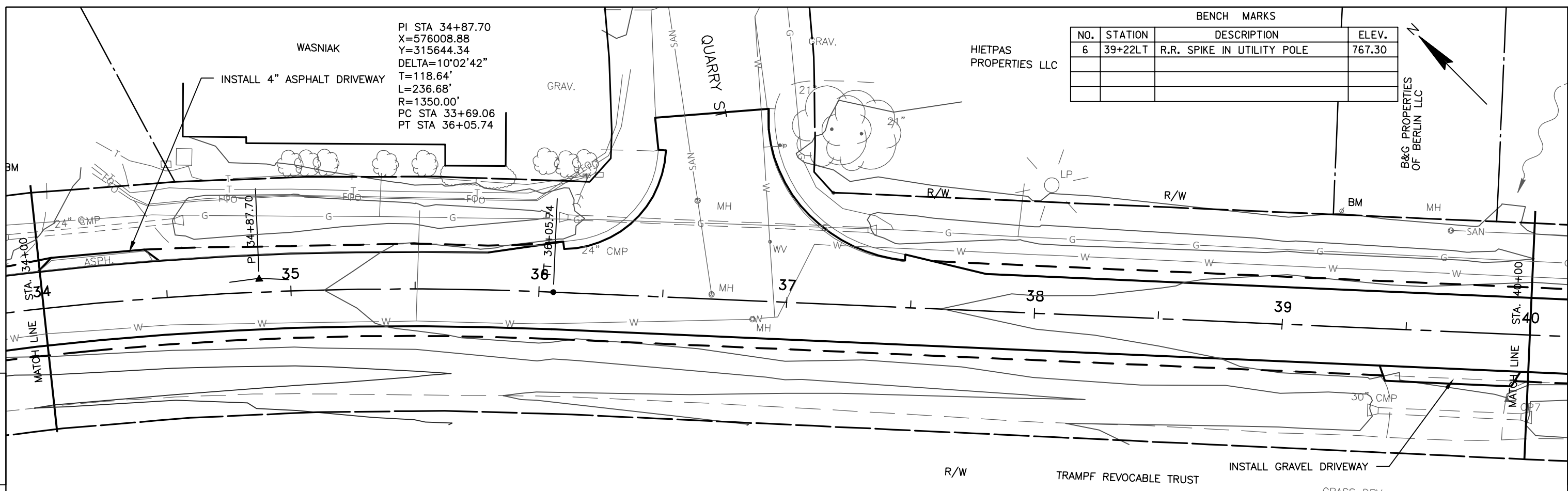
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BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
5	33+86LT	R.R. SPIKE IN UTILITY POLE	762.42

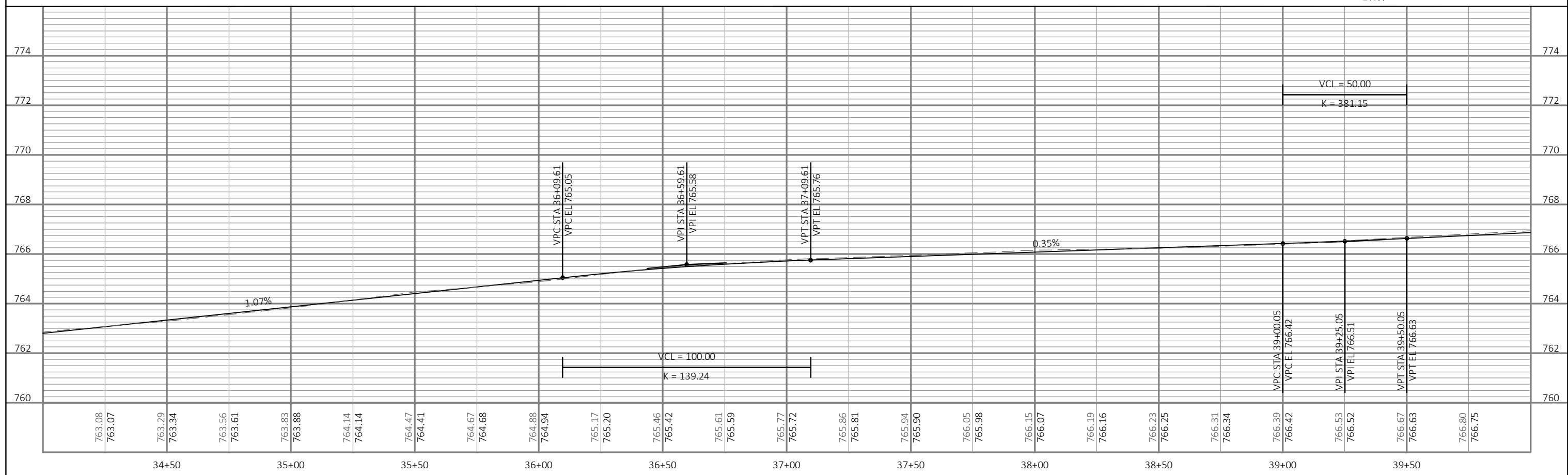


PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE PLAN AND PROFILE: RIPON ROAD RESURFACING SHEET: E

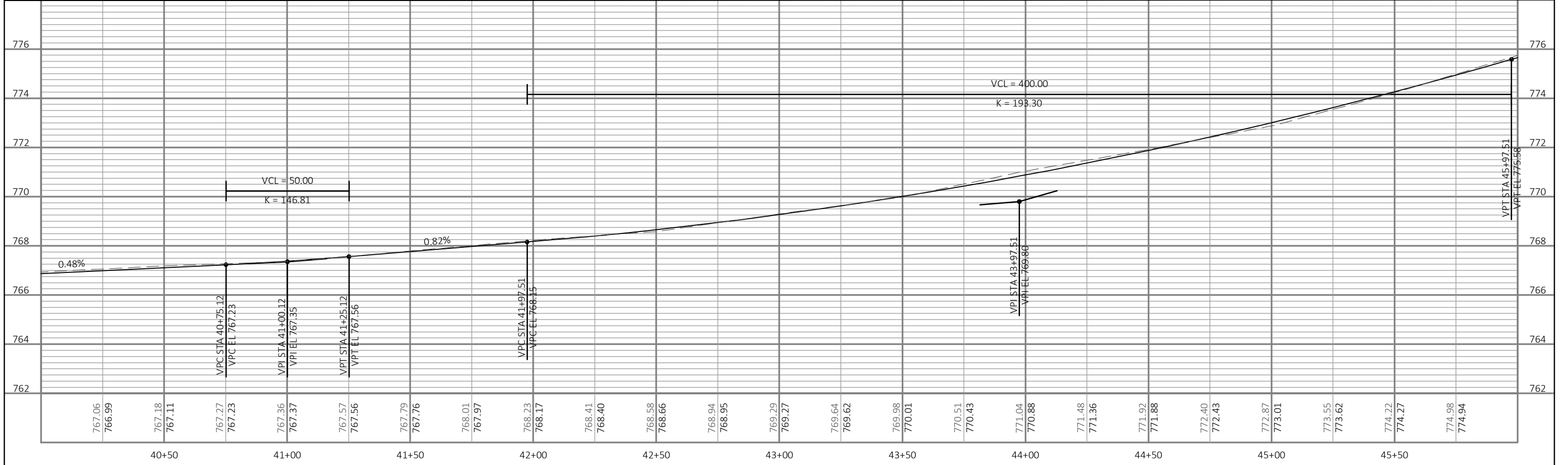
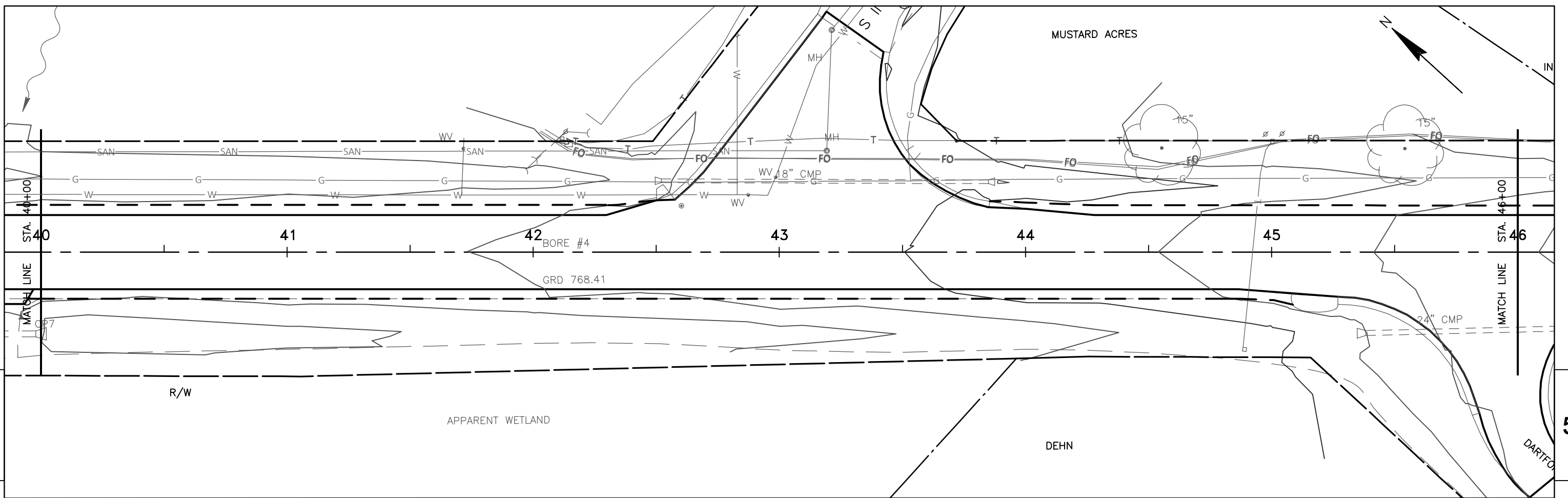


BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
6	39+22LT	R.R. SPIKE IN UTILITY POLE	767.30

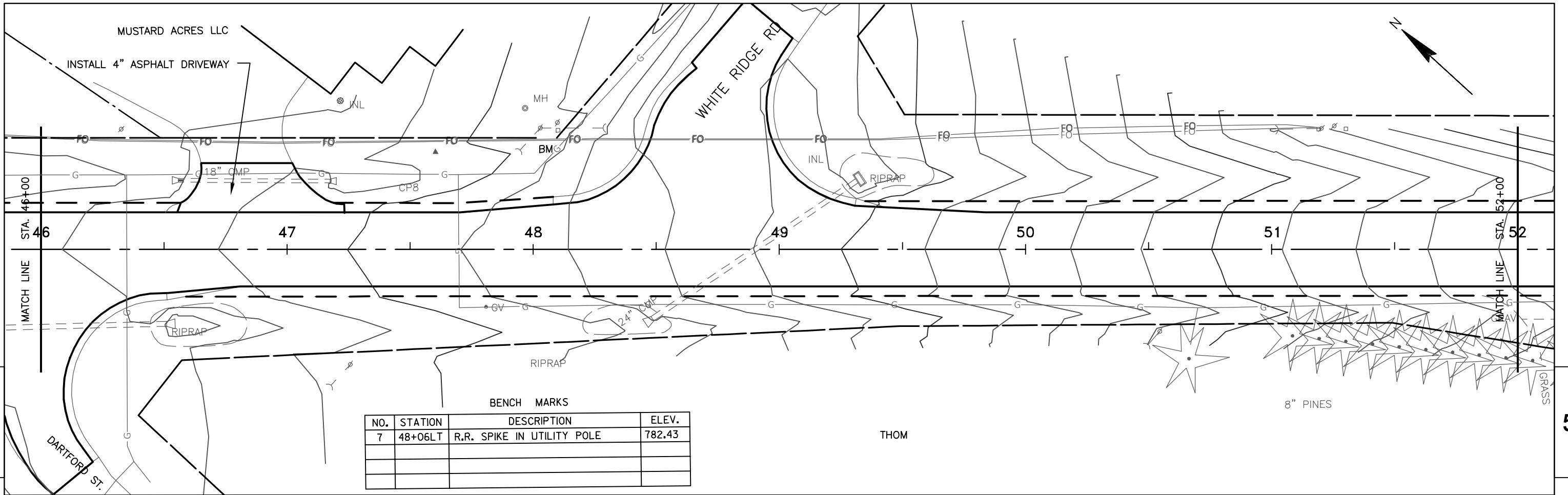
PI STA 34+87.70
 X=576008.88
 Y=315644.34
 DELTA=10°02'42"
 T=118.64'
 L=236.68'
 R=1350.00'
 PC STA 33+69.06
 PT STA 36+05.74



PROJECT NO: 6995-09-71	HWY: CITY OF BERLIN, RIPON ROAD	COUNTY: GREEN LAKE	PLAN AND PROFILE: RIPON ROAD RESURFACING	SHEET E
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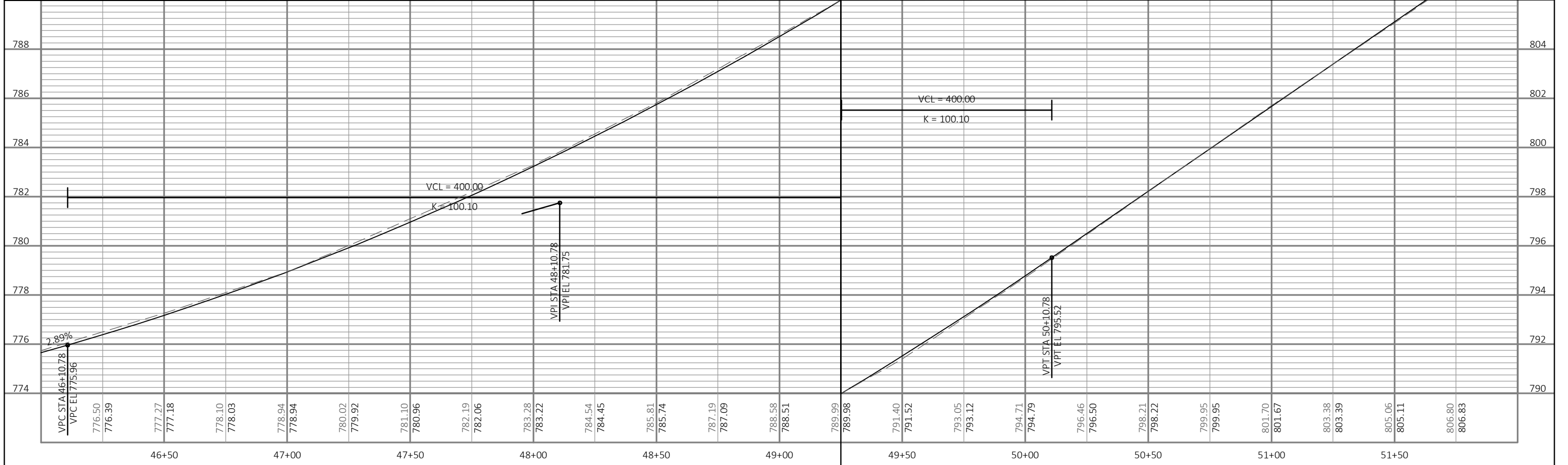


PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE PLAN AND PROFILE: RIPON ROAD RESURFACING SHEET E



BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
7	48+06LT	R.R. SPIKE IN UTILITY POLE	782.43

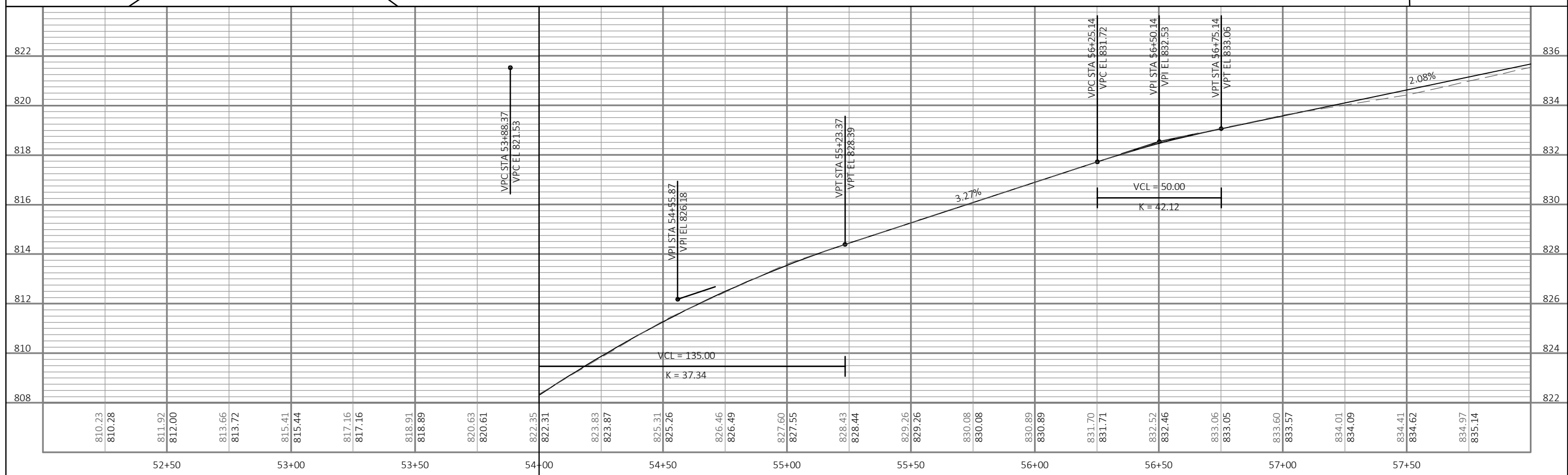
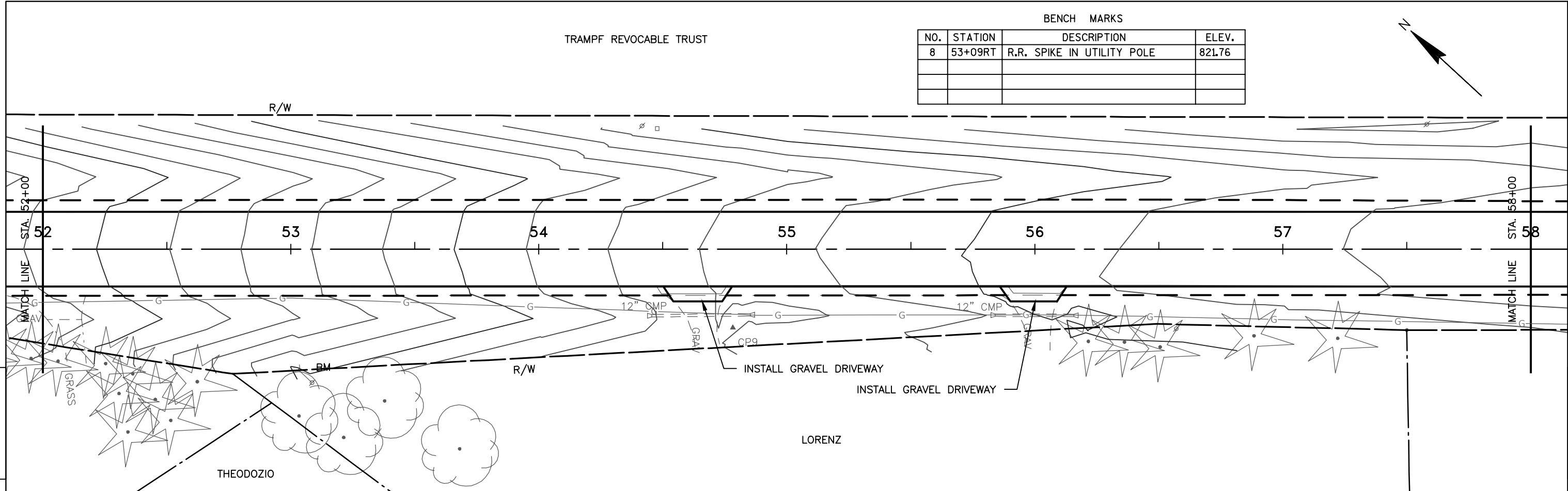
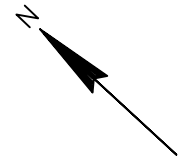


PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE PLAN AND PROFILE: RIPON ROAD RESURFACING SHEET **E**

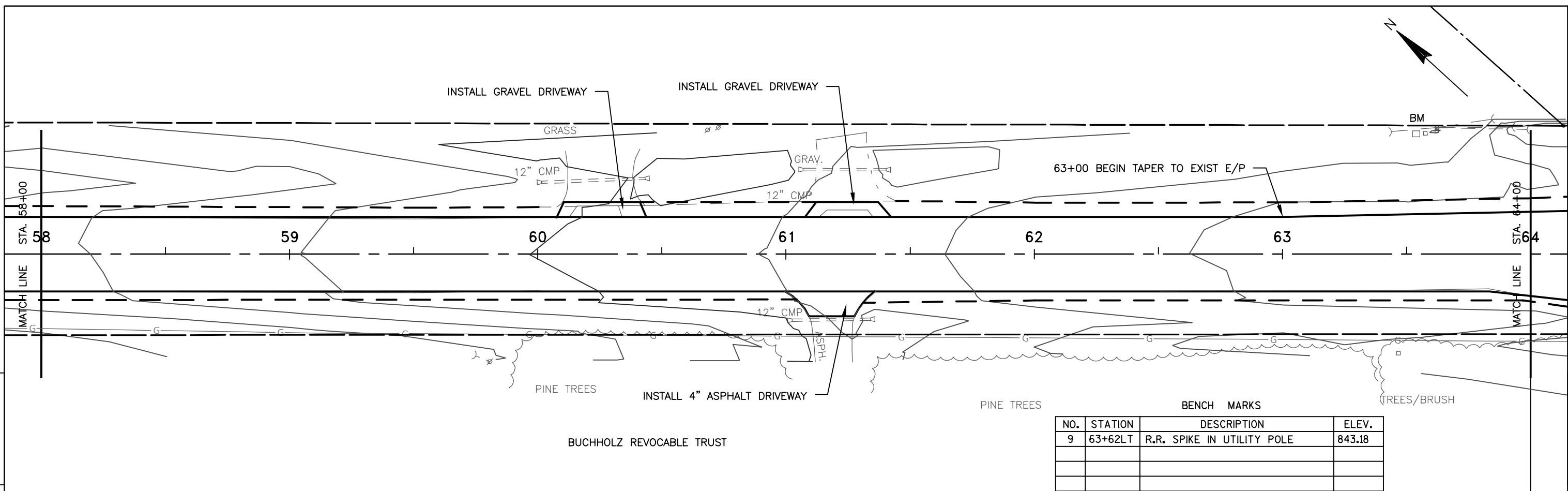
TRAMPF REVOCABLE TRUST

BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
8	53+09RT	R.R. SPIKE IN UTILITY POLE	821.76

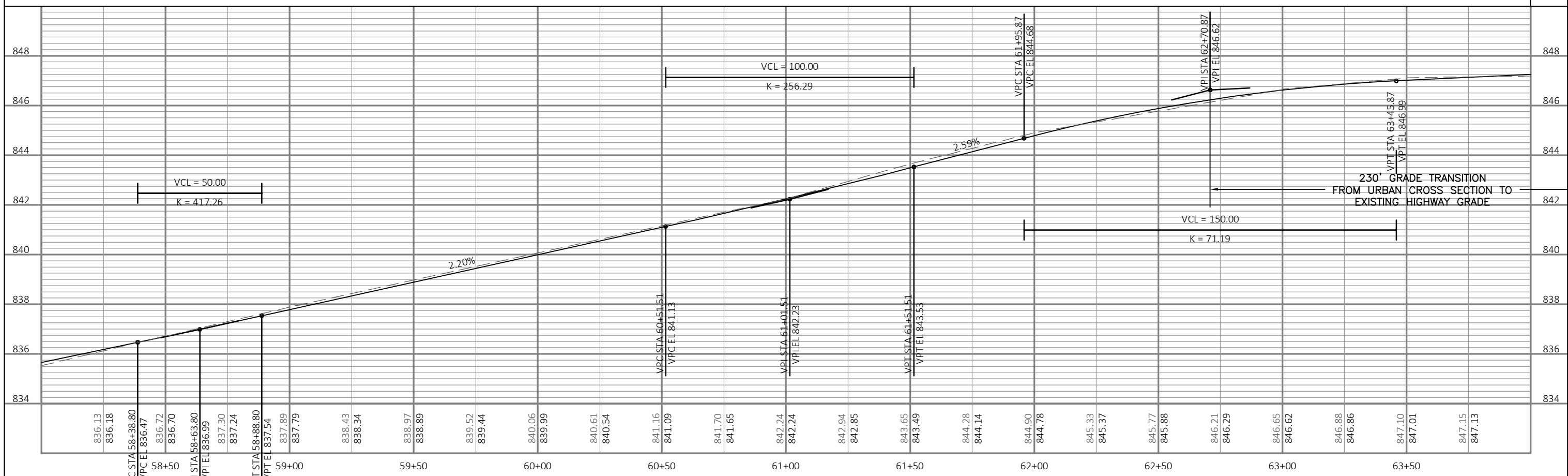


PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE PLAN AND PROFILE: RIPON ROAD RESURFACING SHEET **E**

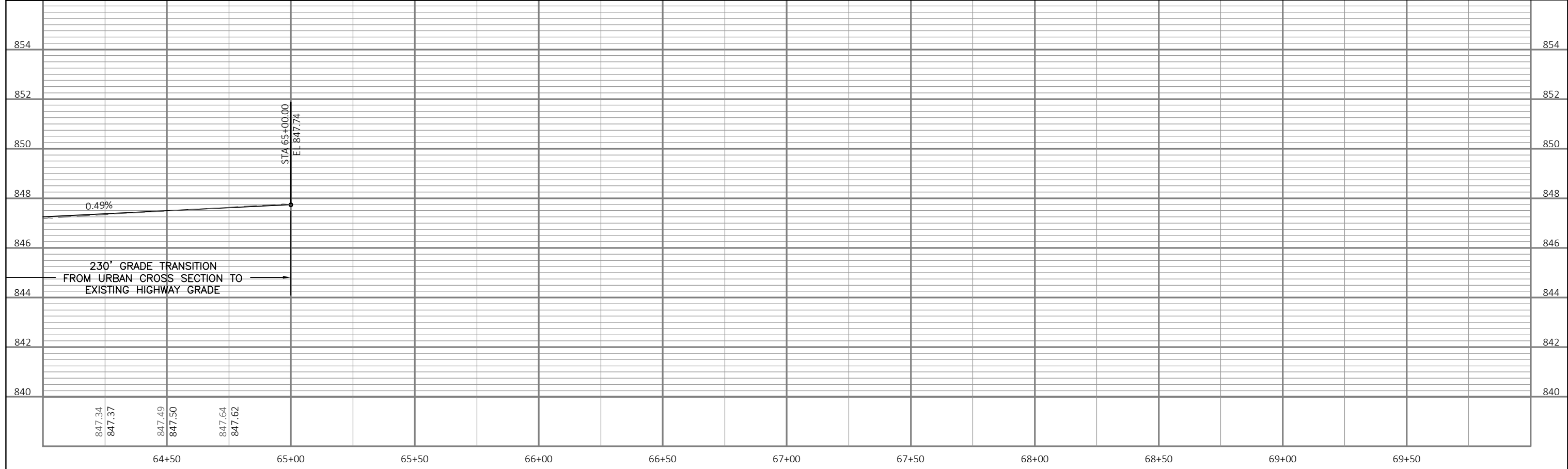
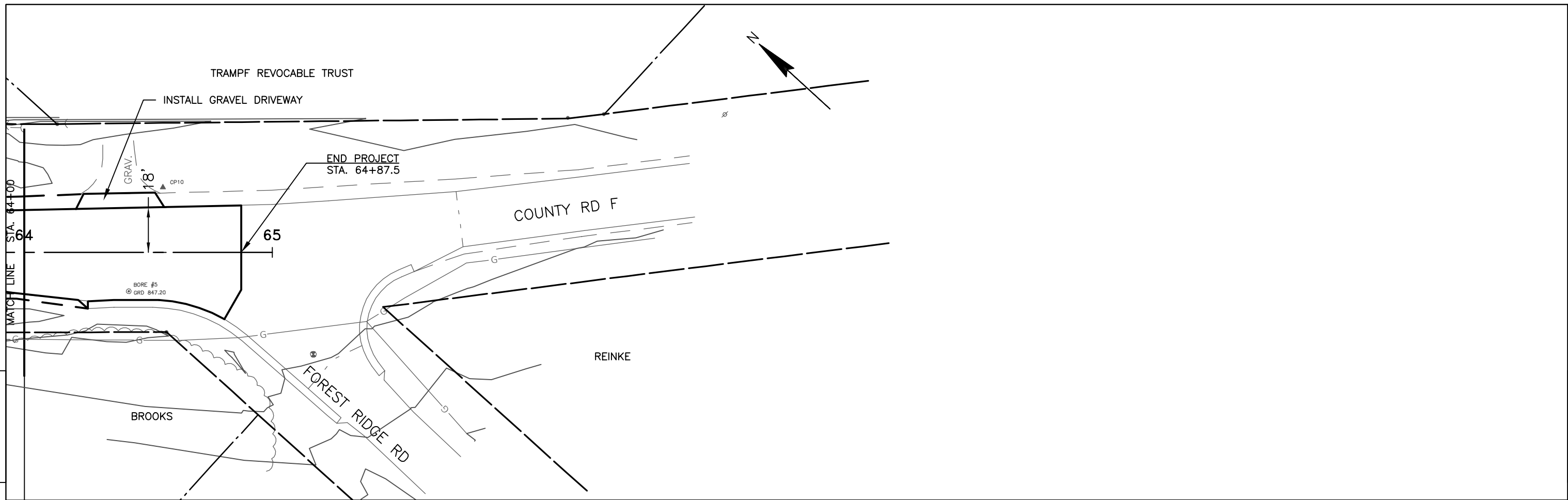


BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
9	63+62LT	R.R. SPIKE IN UTILITY POLE	843.18



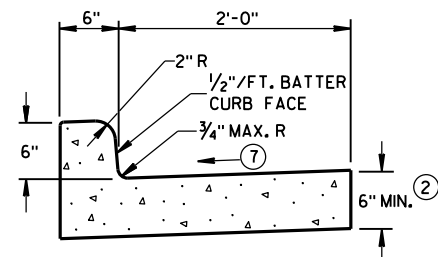
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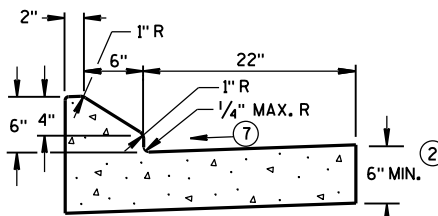
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Standard Detail Drawing List

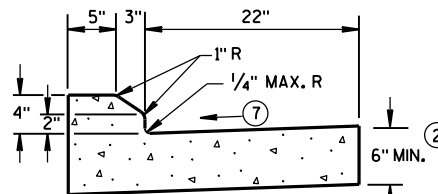
08D01-20A	CONCRETE CURB & GUTTER
08D01-20B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C02-07A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-07B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C33-03	STOP LINE AND CROSSWALK PAVEMENT MARKING



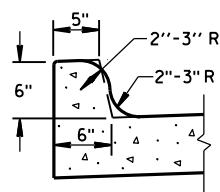
TYPES A^① & D



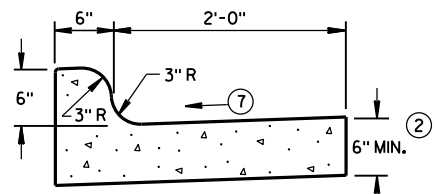
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

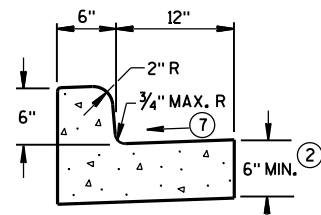


TYPES K^① & L
(OPTIONAL CURB SHAPE)



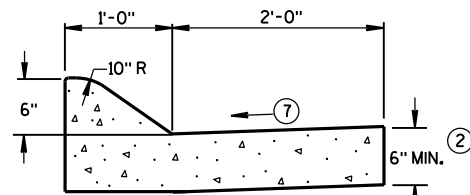
TYPES K^① & L

CONCRETE CURB & GUTTER 30"

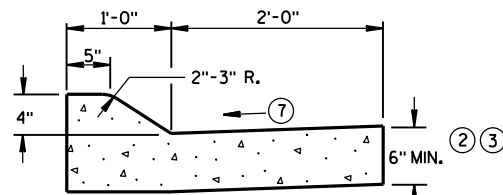


TYPES A^① & D

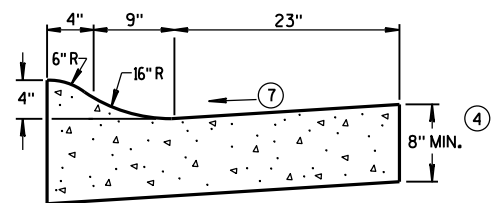
CONCRETE CURB & GUTTER 18"



6" SLOPED CURB TYPES A^① & D

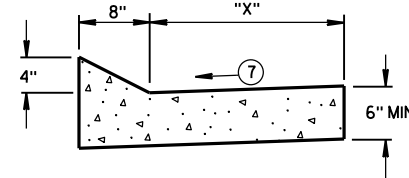


4" SLOPED CURB TYPES A^① & D



4" SLOPED CURB TYPES R^① & T^⑤

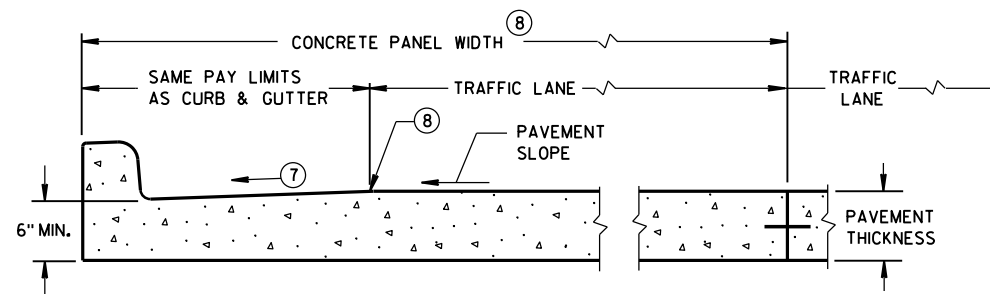
CONCRETE CURB & GUTTER 36"



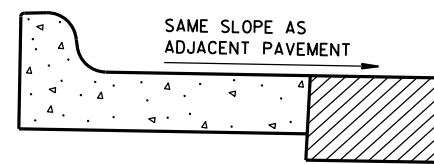
TYPES TBT & TBTT^①

CONCRETE CURB & GUTTER

TBT & TBTT	"X"
30"	22"
36"	28"



PARTIAL SECTION OF PAVEMENT
WITH INTEGRAL CURB & GUTTER



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

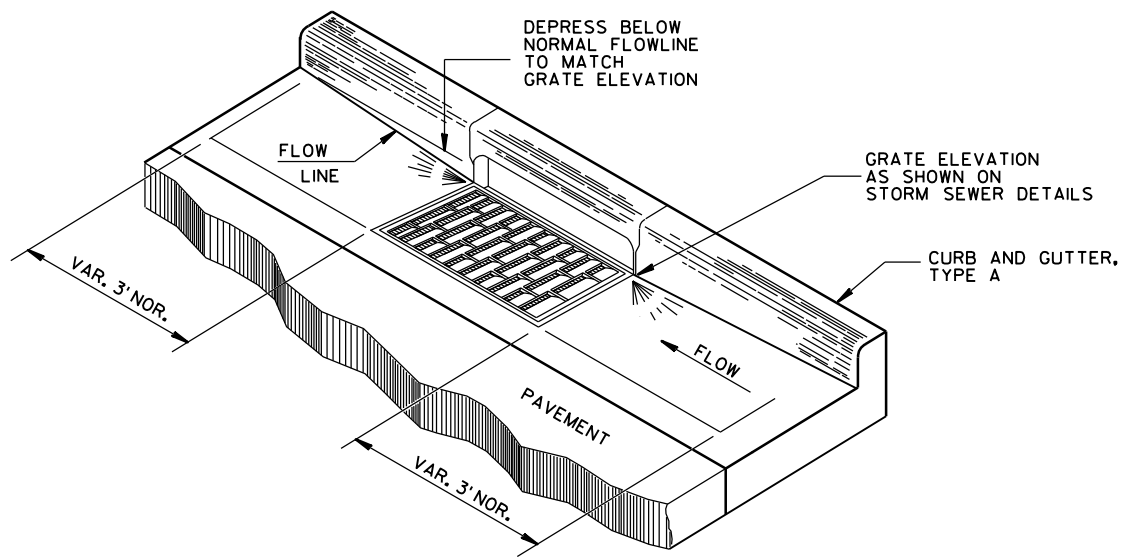
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'

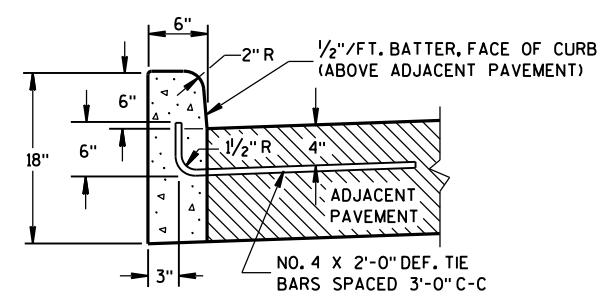
* BIKE LANE IS NOT SHOWN.

CONCRETE CURB & GUTTER

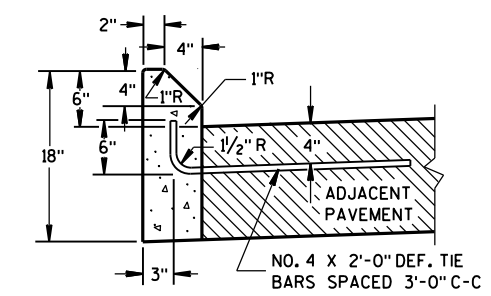
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



DETAIL OF CURB AND GUTTER AT INLETS
(TYPE H INLET COVER SHOWN)



TYPES A^① & D

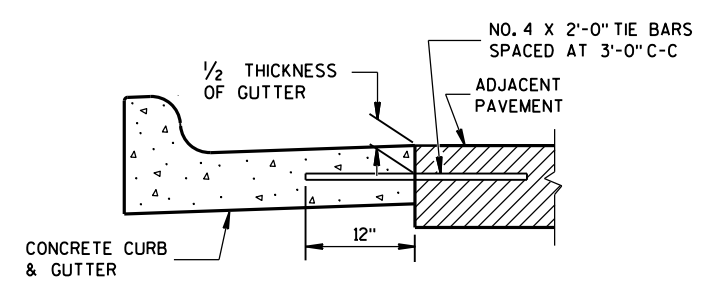


TYPES G^① & J

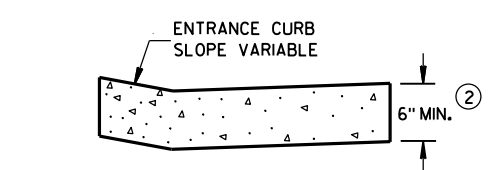
GENERAL NOTES

- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.
- PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.
- UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.
 - ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
 - ③ REFER TO SDD 8D18 AND SDD 8D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

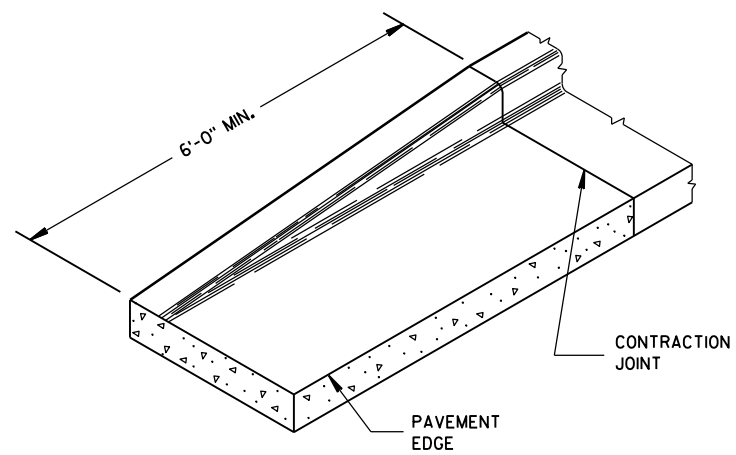
CONCRETE CURB



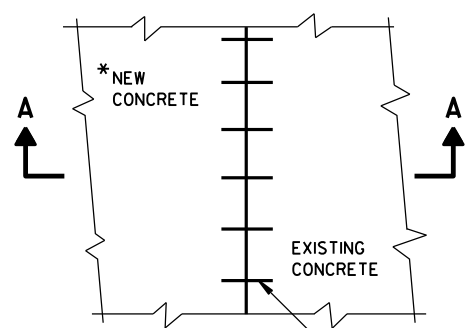
TYPICAL TIE BAR LOCATION^①



DRIVEWAY ENTRANCE CURB^⑨
(WHEN DIRECTED BY THE ENGINEER)



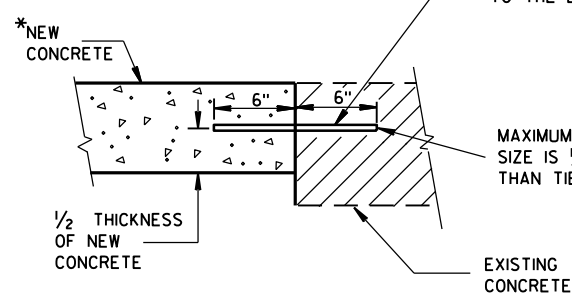
END SECTION CURB & GUTTER



PLAN VIEW

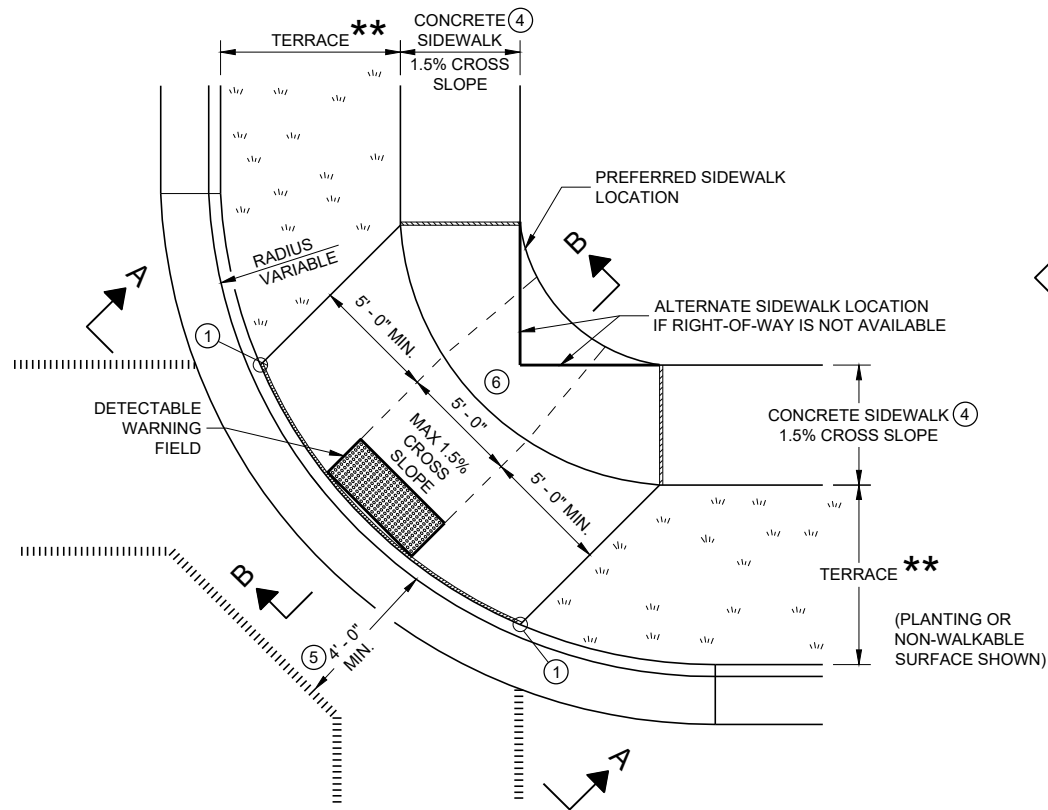
*NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

NO. 6 TIE BARS SPACED 2'-6" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT.

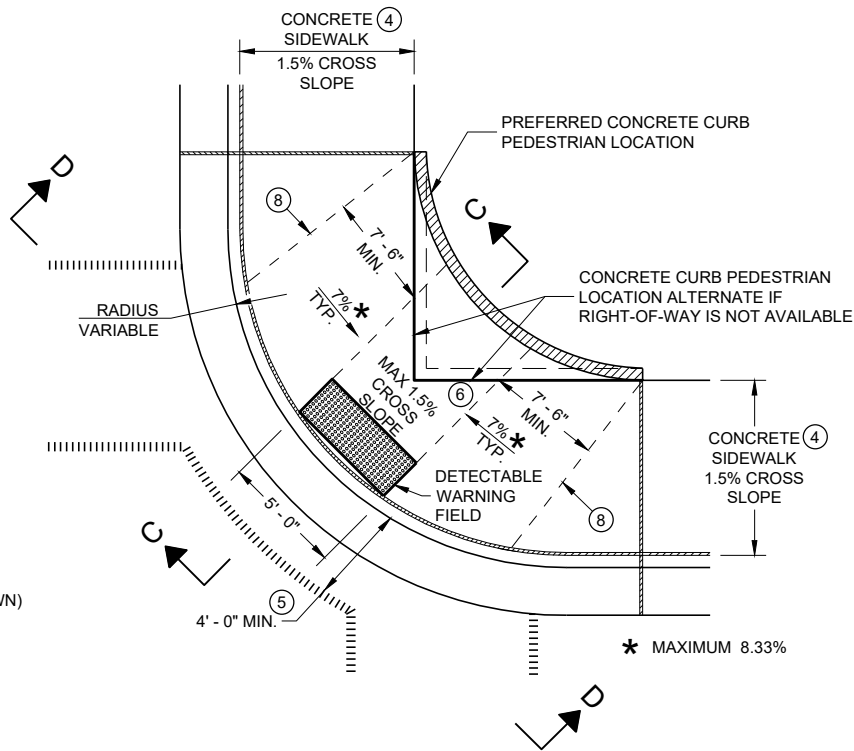


SECTION A-A
TIE BARS DRILLED INTO EXISTING PAVEMENT

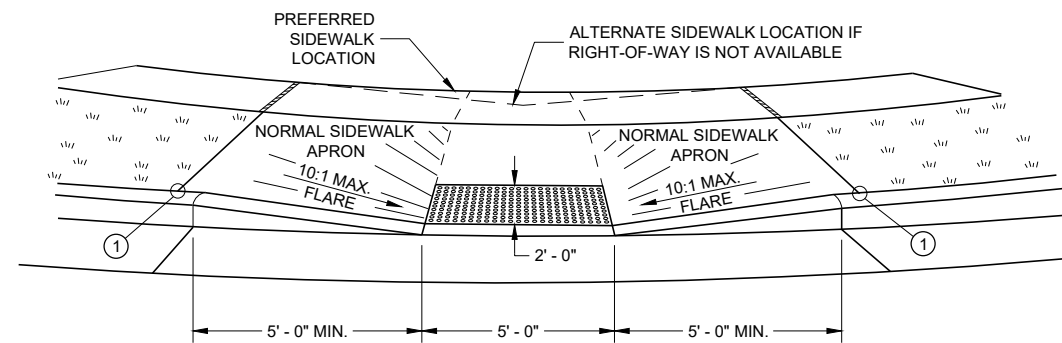
CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June, 2017 DATE	/s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	



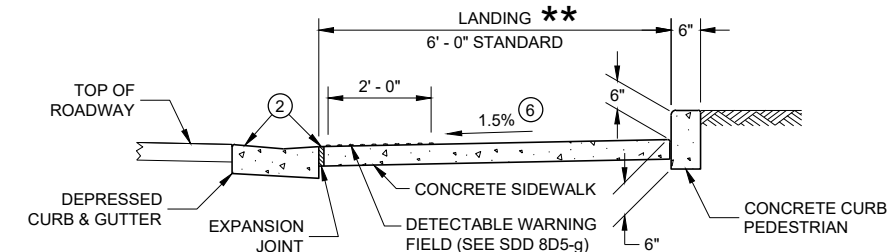
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



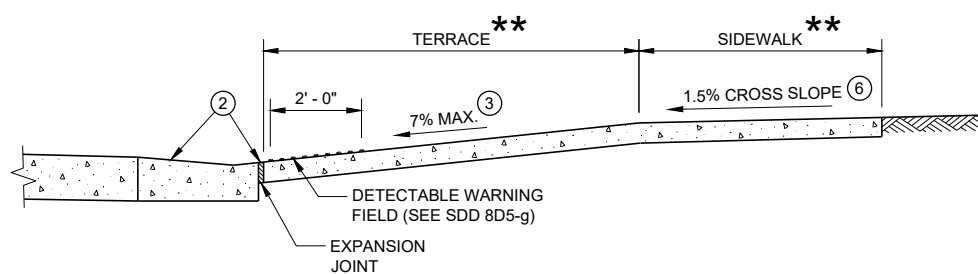
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



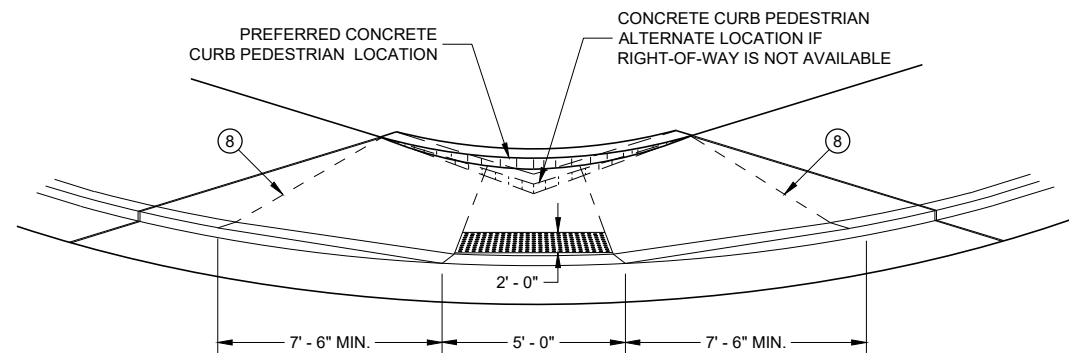
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
 DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
 TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

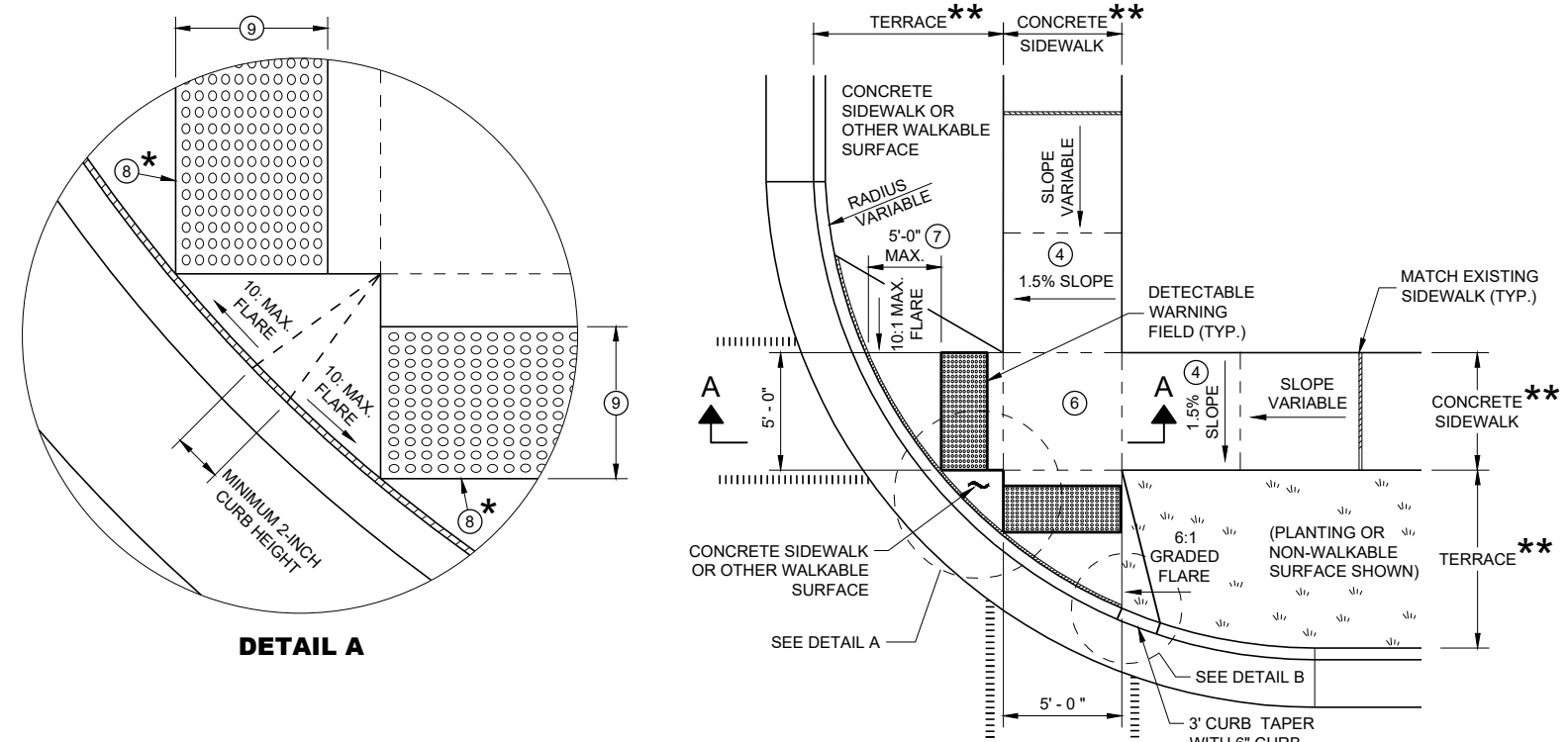
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

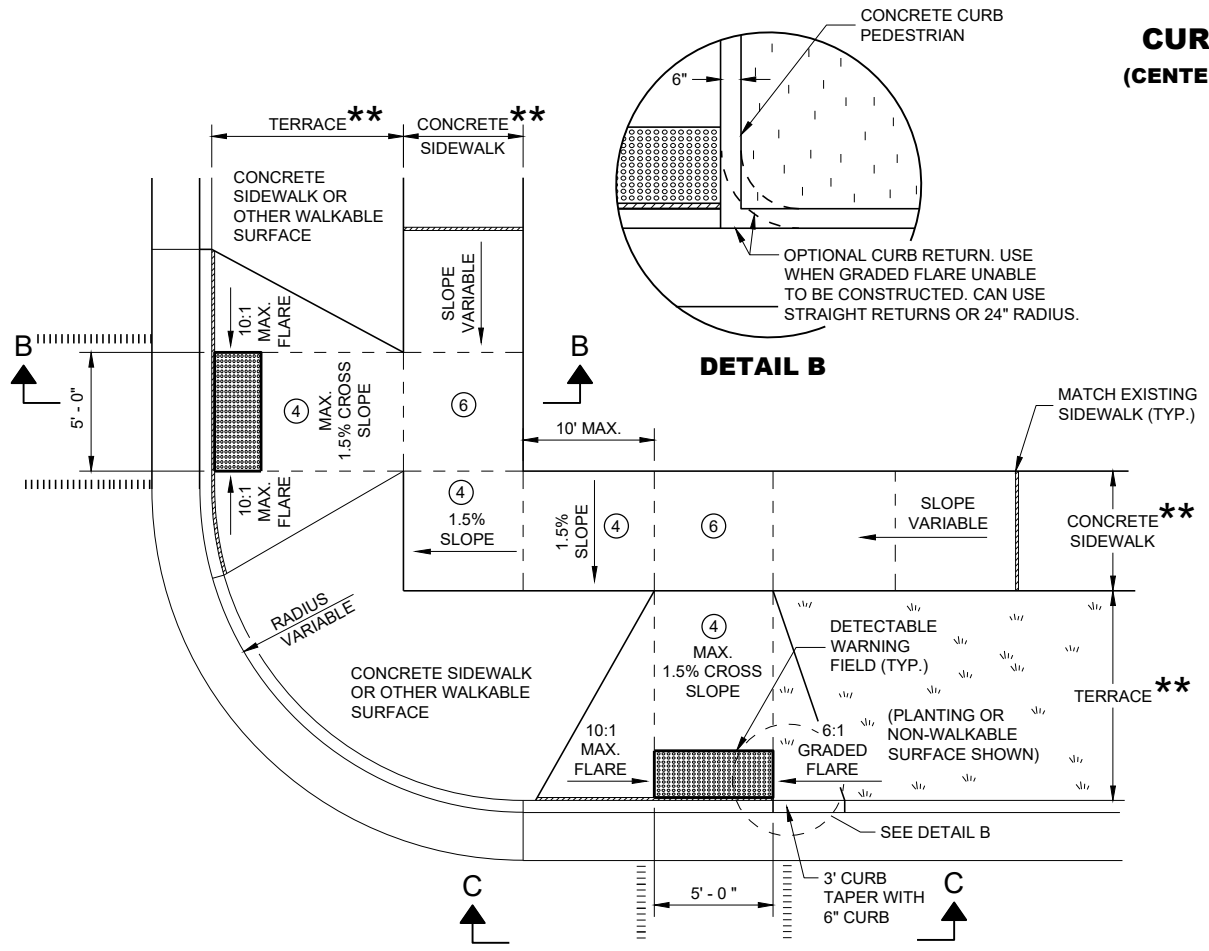
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

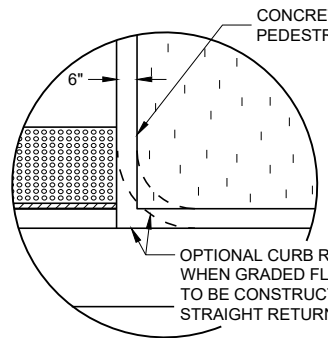
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



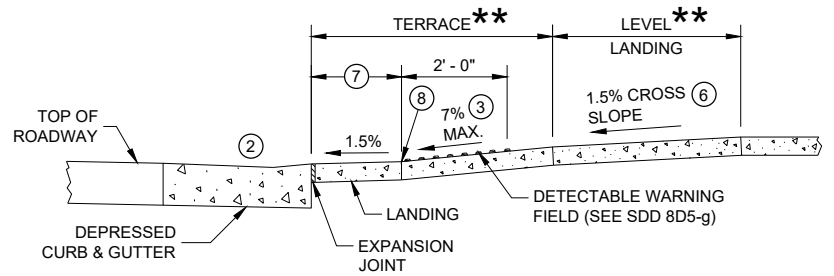
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



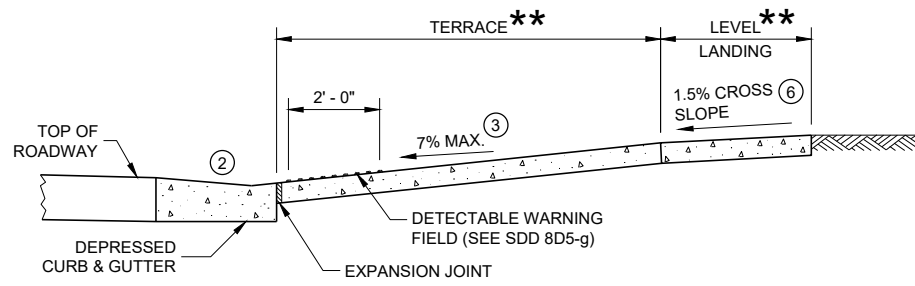
DETAIL B

GENERAL NOTES

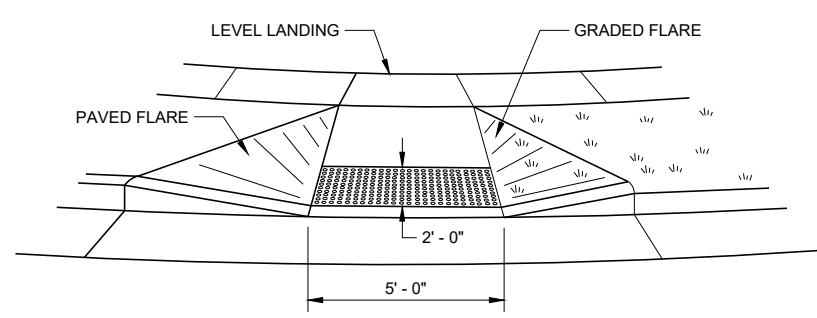
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

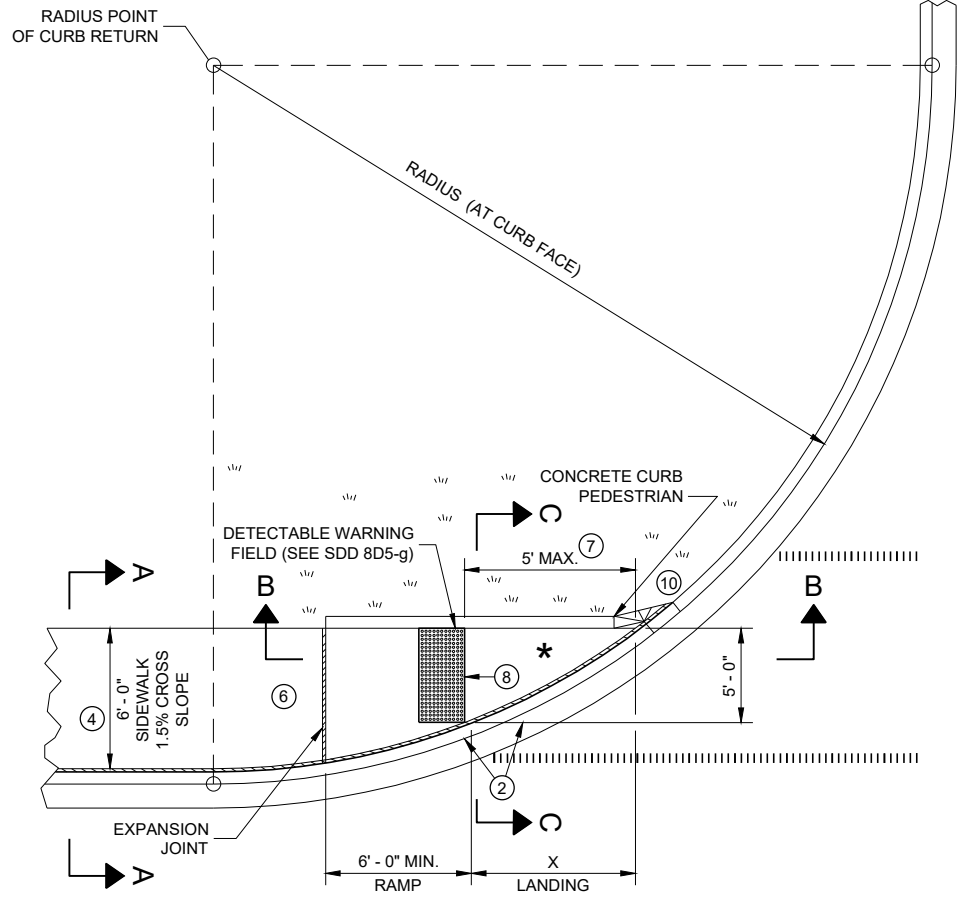
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

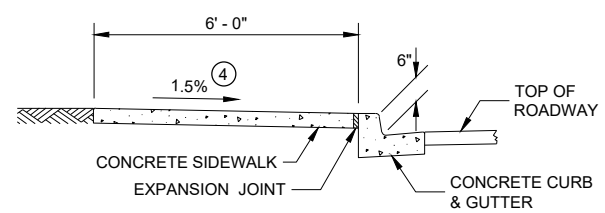
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 4A

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



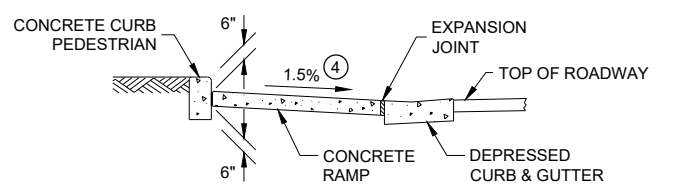
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

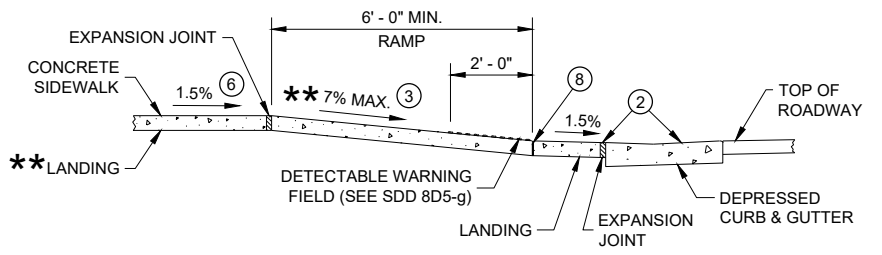
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



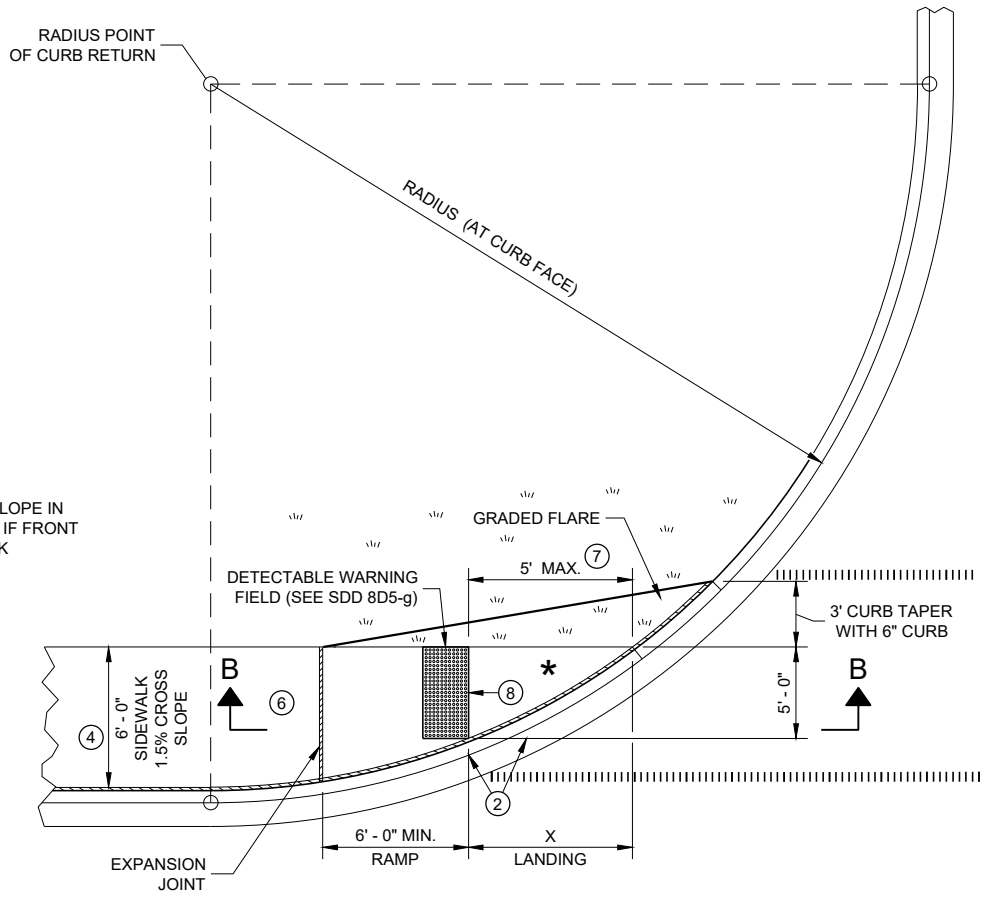
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

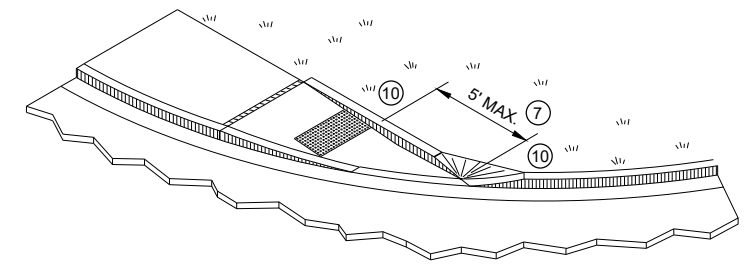


SECTION B - B FOR TYPE 4A AND TYPE 4A1

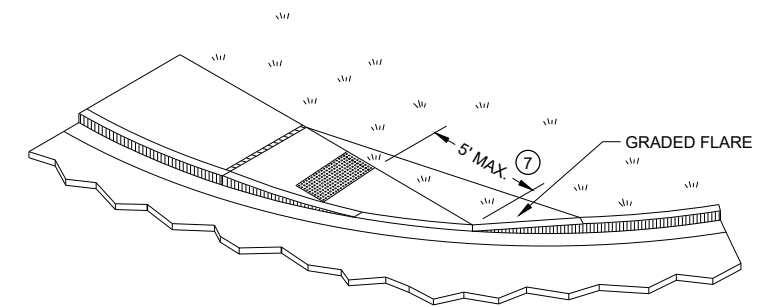
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



PLAN VIEW CURB RAMP TYPE 4A1



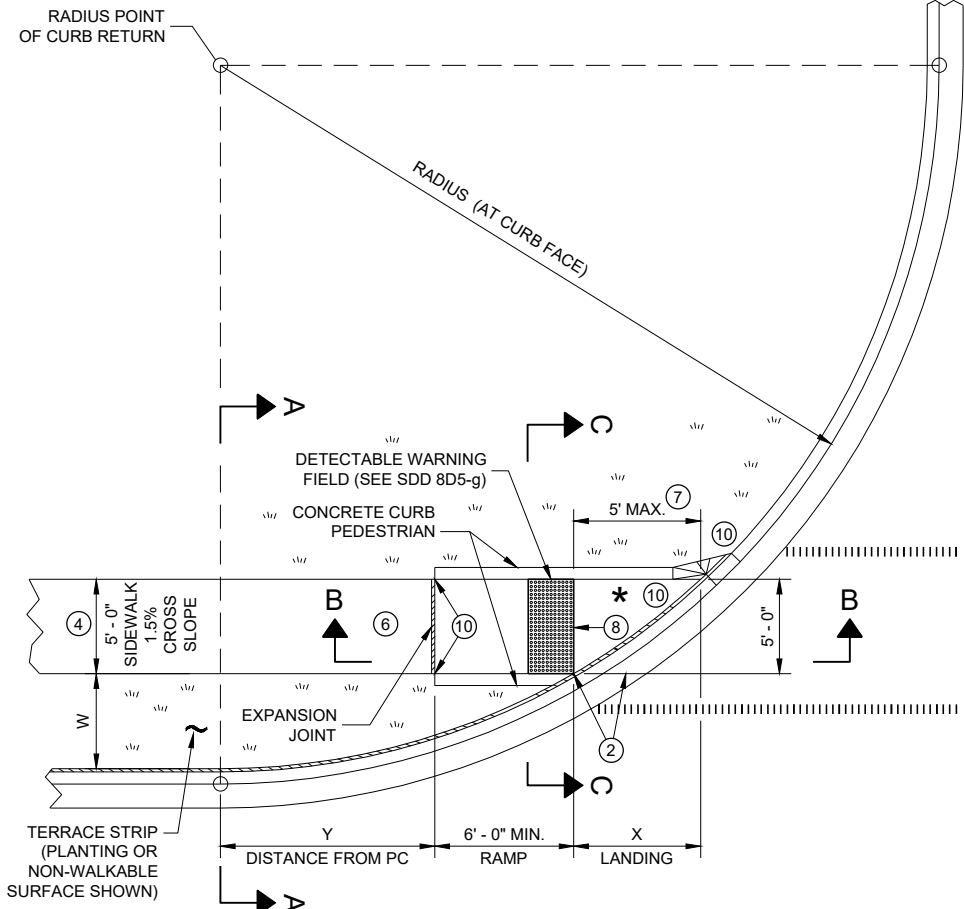
ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

CURB RAMPS TYPE 4A AND 4A1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

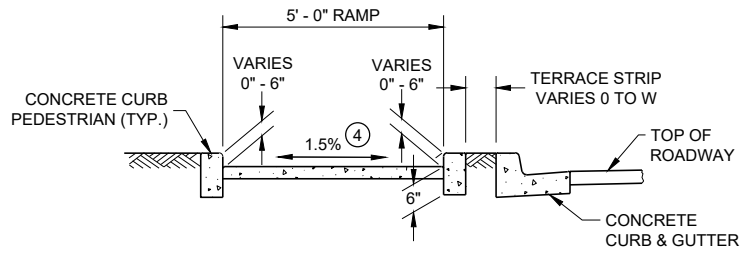
INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

LEGEND

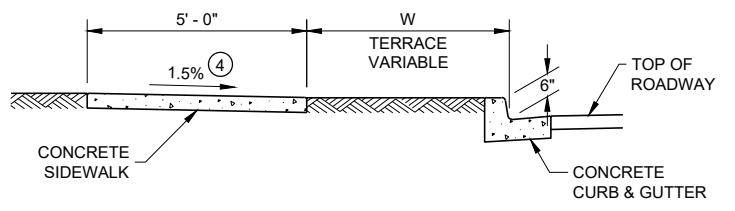
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

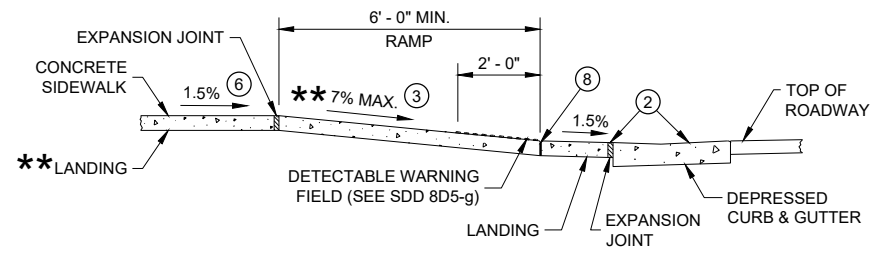


SECTION C - C FOR TYPE 4B



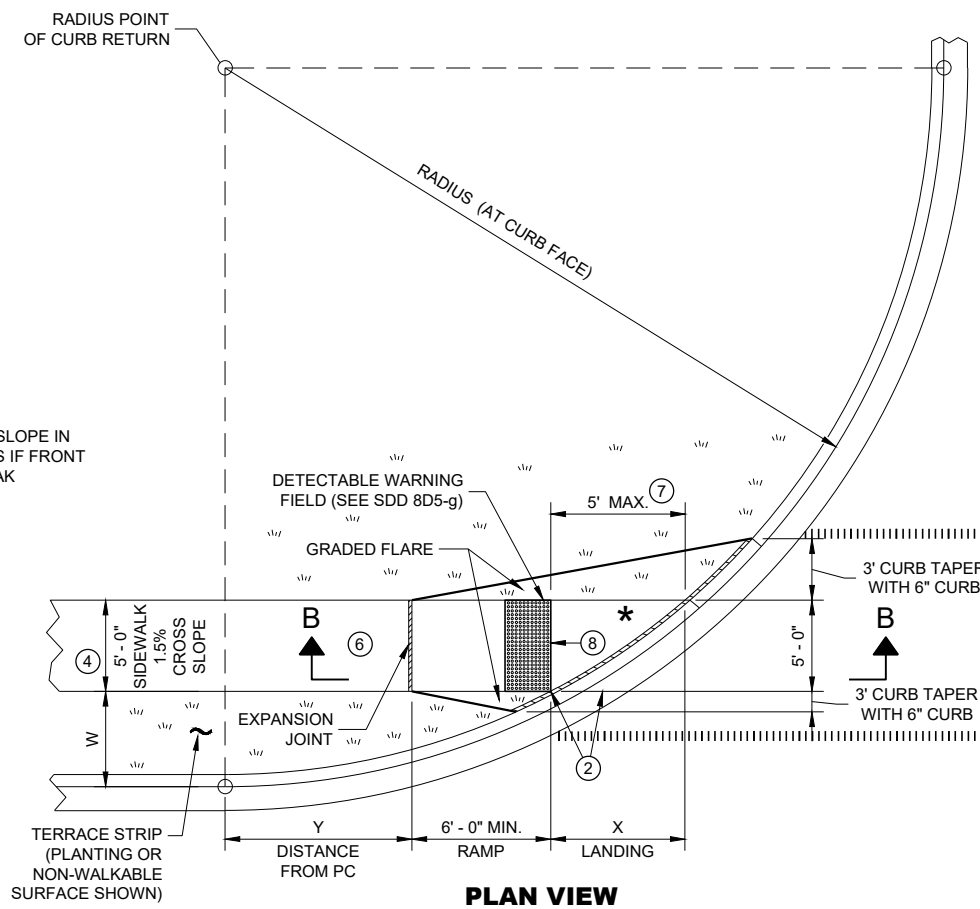
SECTION A - A FOR TYPE 4B

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

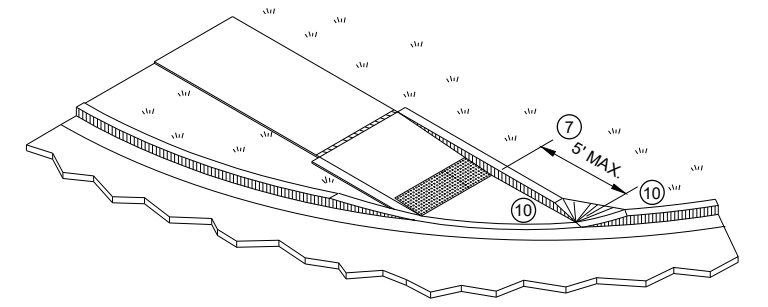


SECTION B - B FOR TYPE 4B AND TYPE 4B1

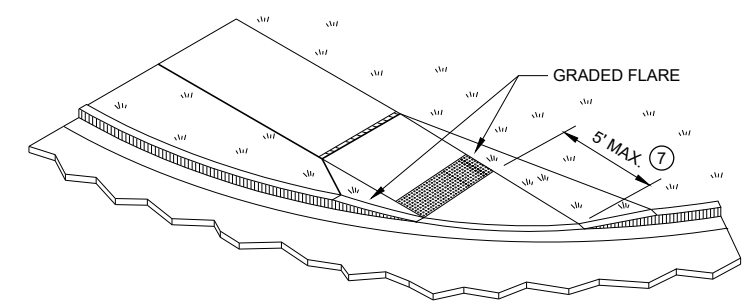
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



PLAN VIEW CURB RAMP TYPE 4B1



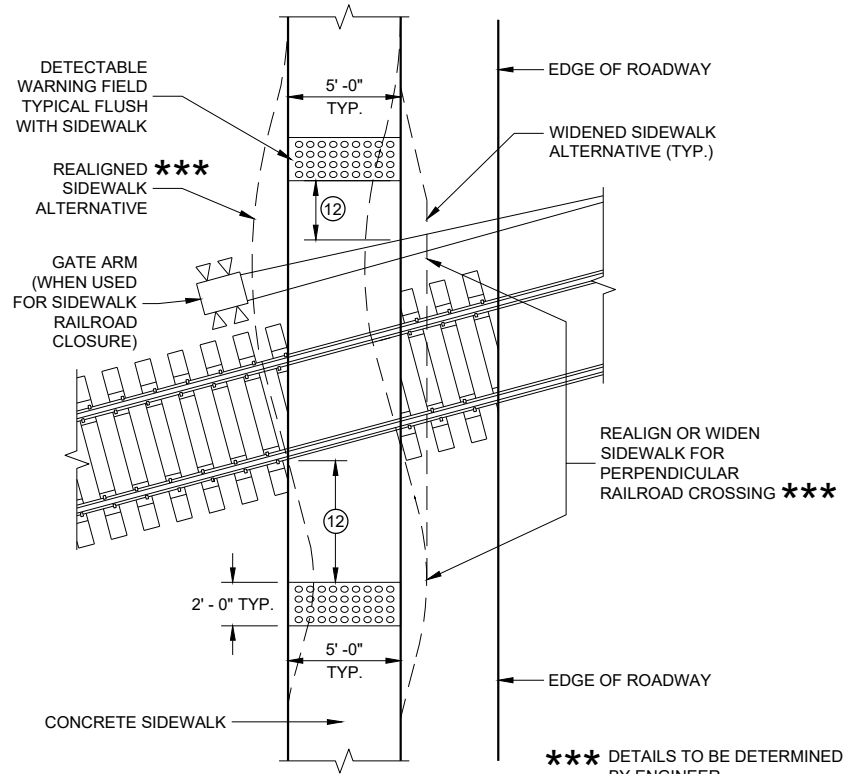
ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS TYPE 4B AND 4B1

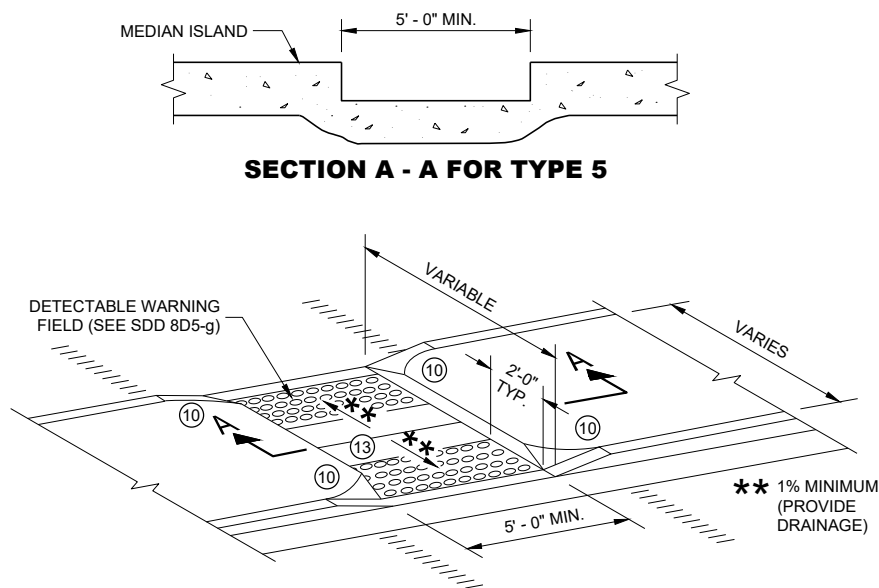
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING

*** DETAILS TO BE DETERMINED BY ENGINEER



CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

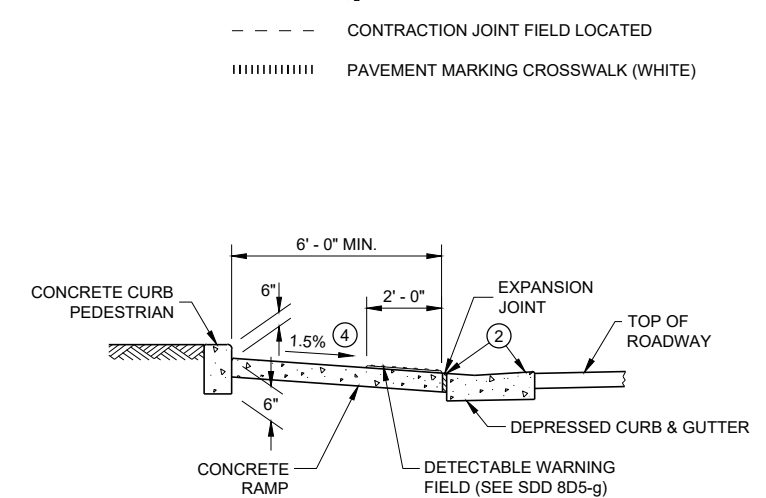
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

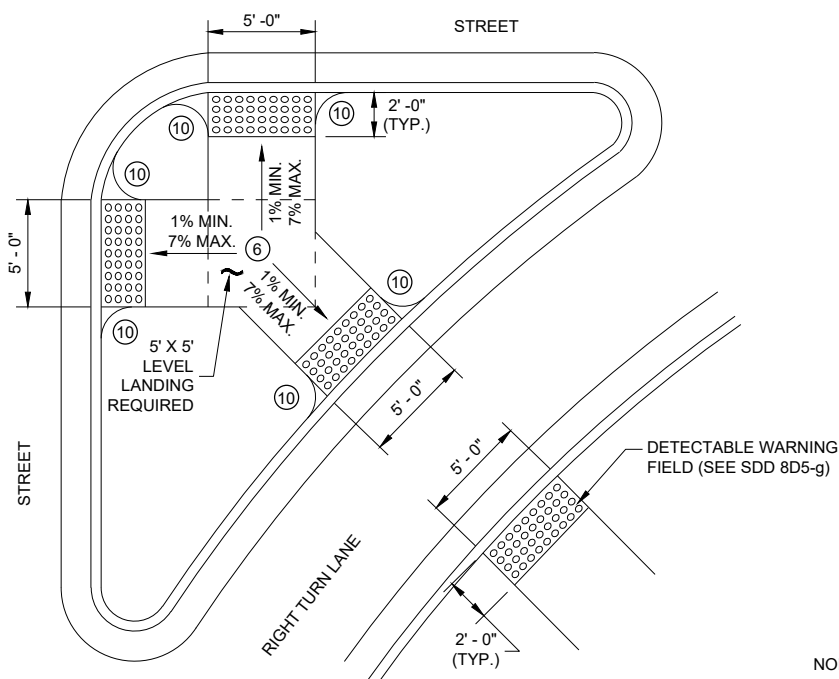
LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



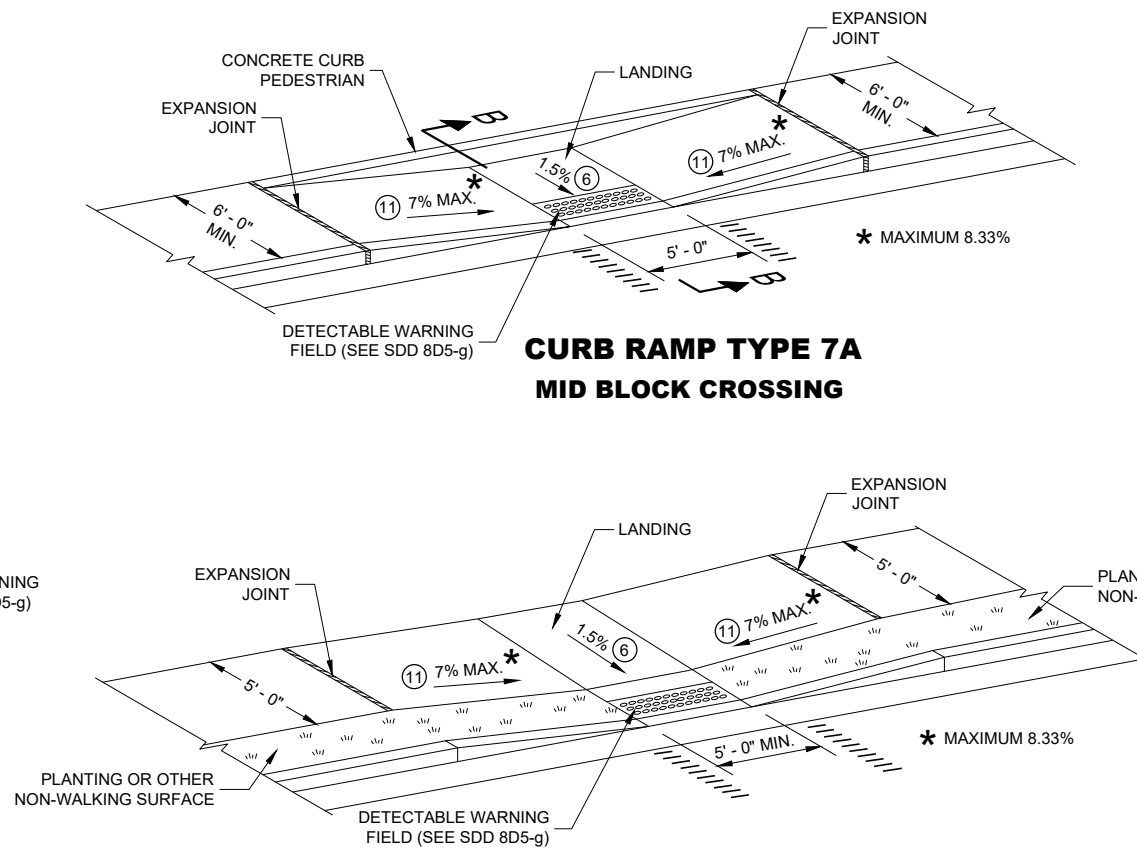
CURB RAMP TYPE 7A
MID BLOCK CROSSING

SECTION B - B FOR TYPE 7A



CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



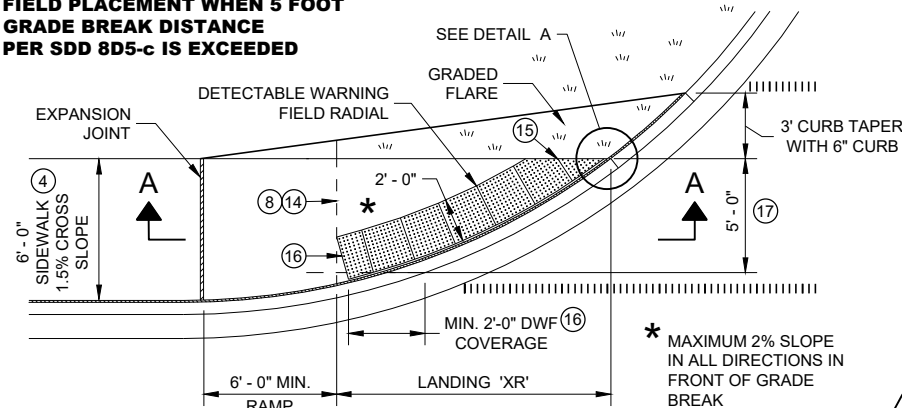
CURB RAMP TYPE 7B
MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

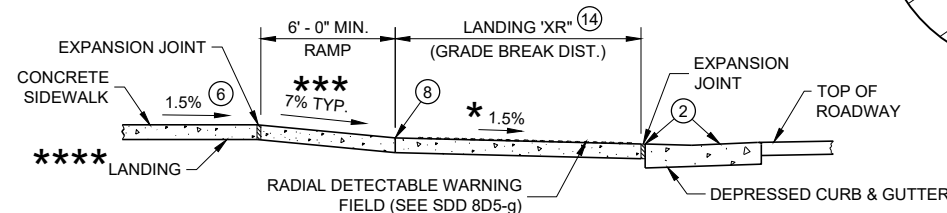
CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

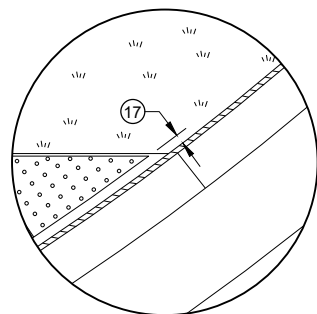


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

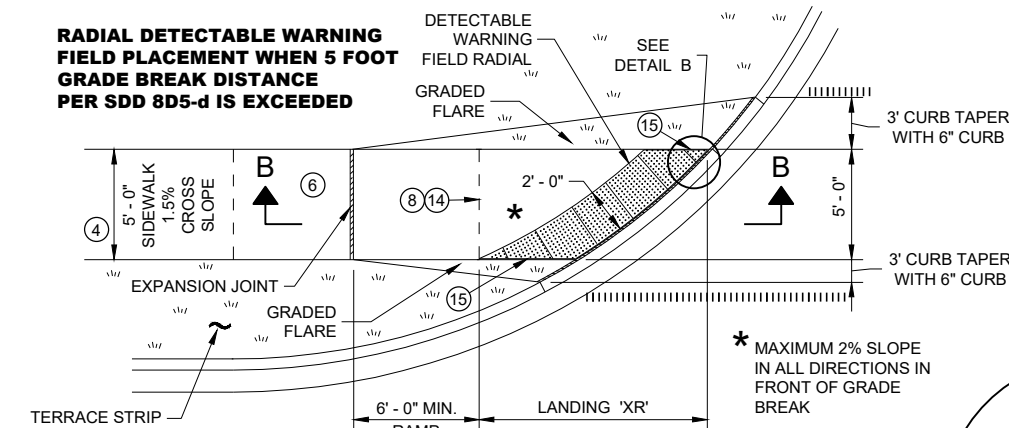


DETAIL A

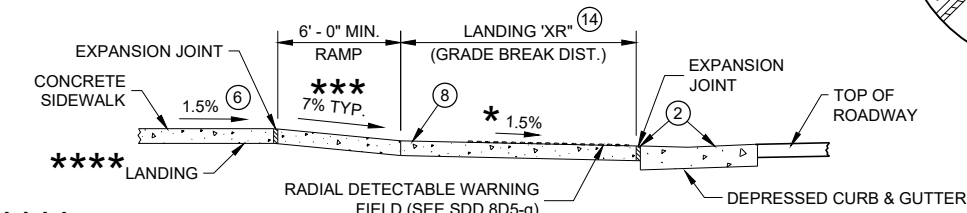
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



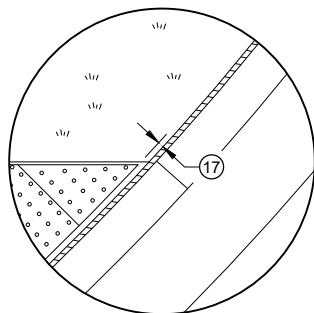
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

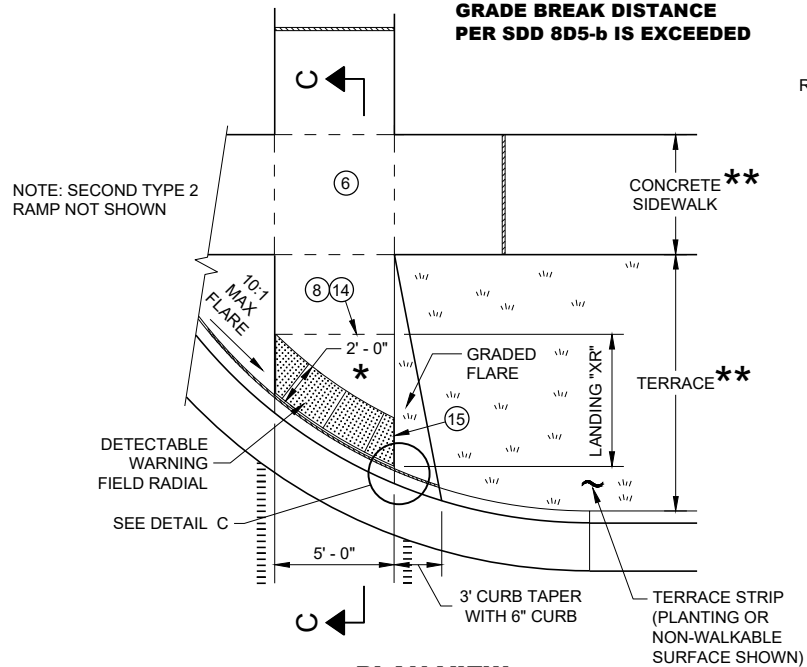
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



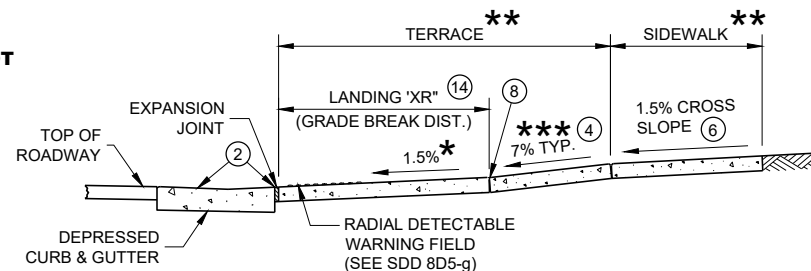
DETAIL B

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)

NOTE: SECOND TYPE 2 RAMP NOT SHOWN

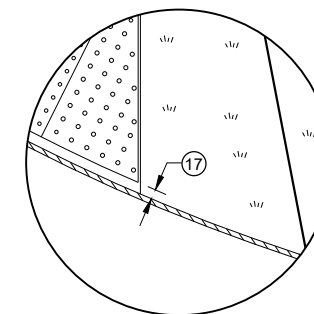


SECTION C - C FOR TYPE 2

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%



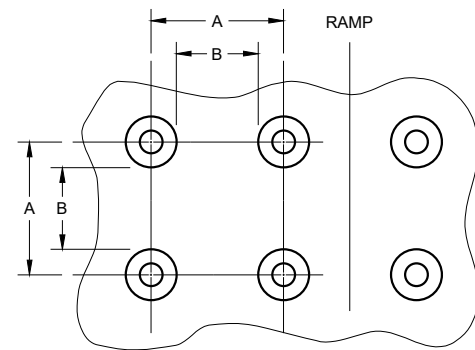
DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

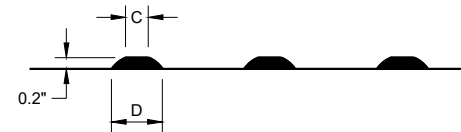
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

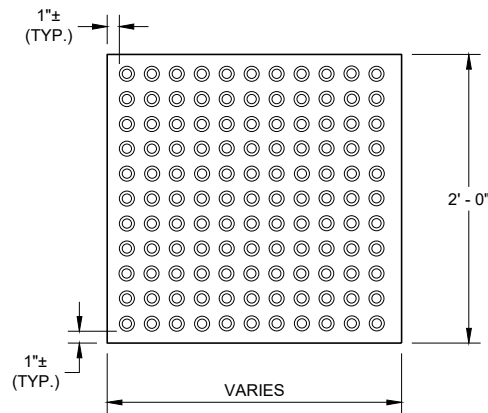


PLAN VIEW

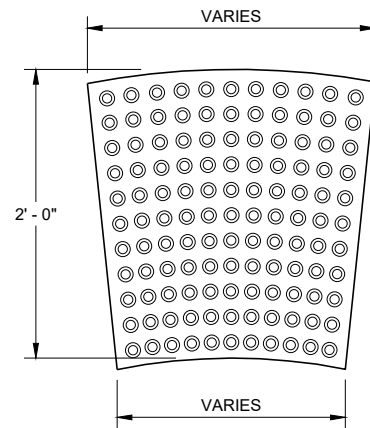


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

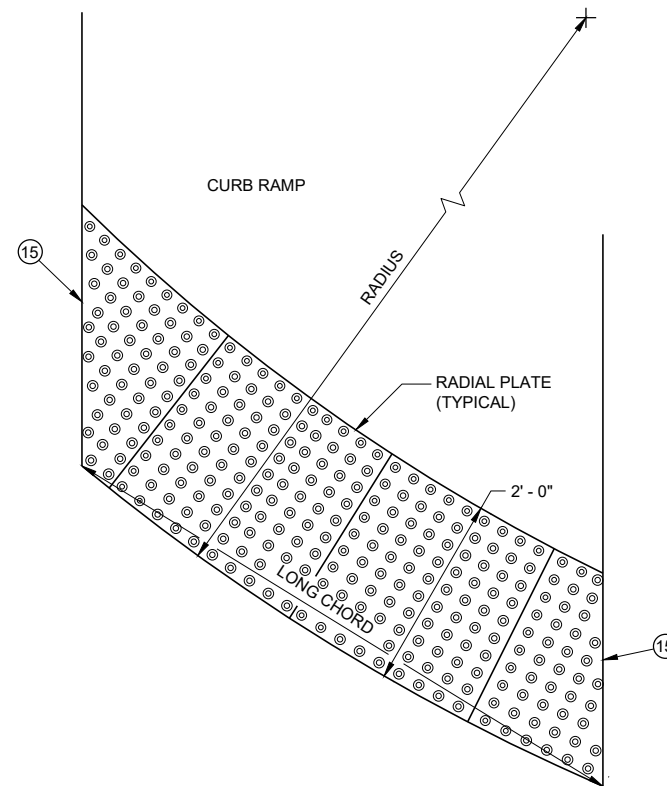


**RECTANGULAR
PLATES**

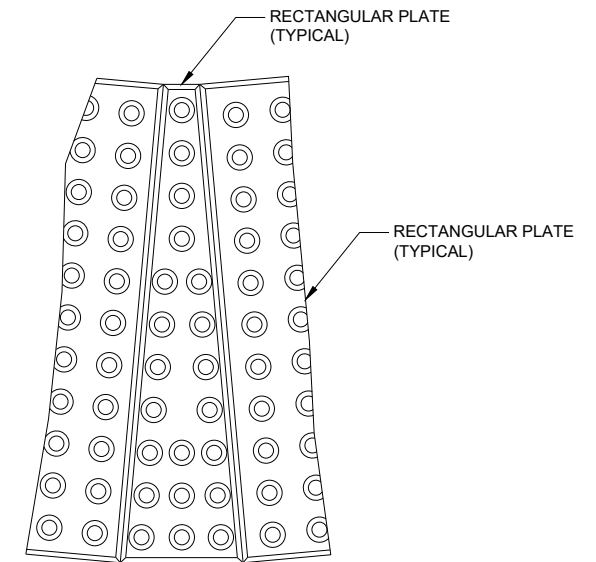


**RADIAL
PLATES**

**PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)**



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**



**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

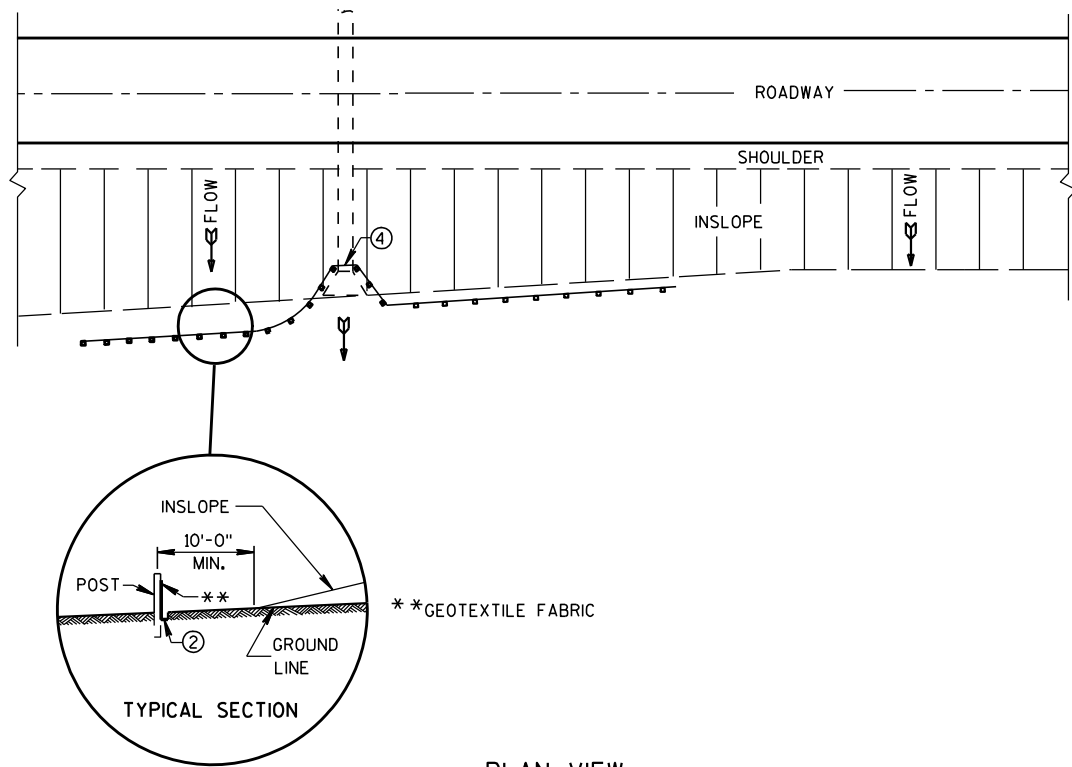
DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

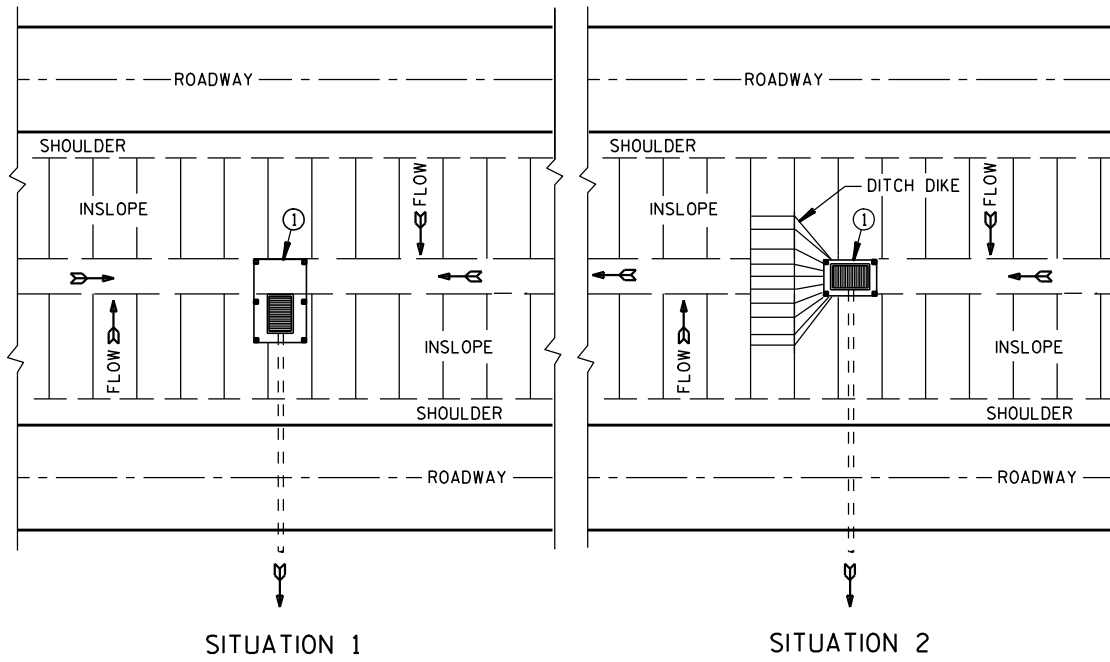
**CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

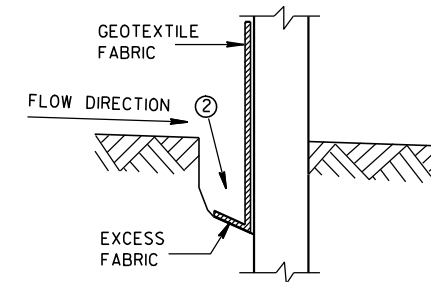


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

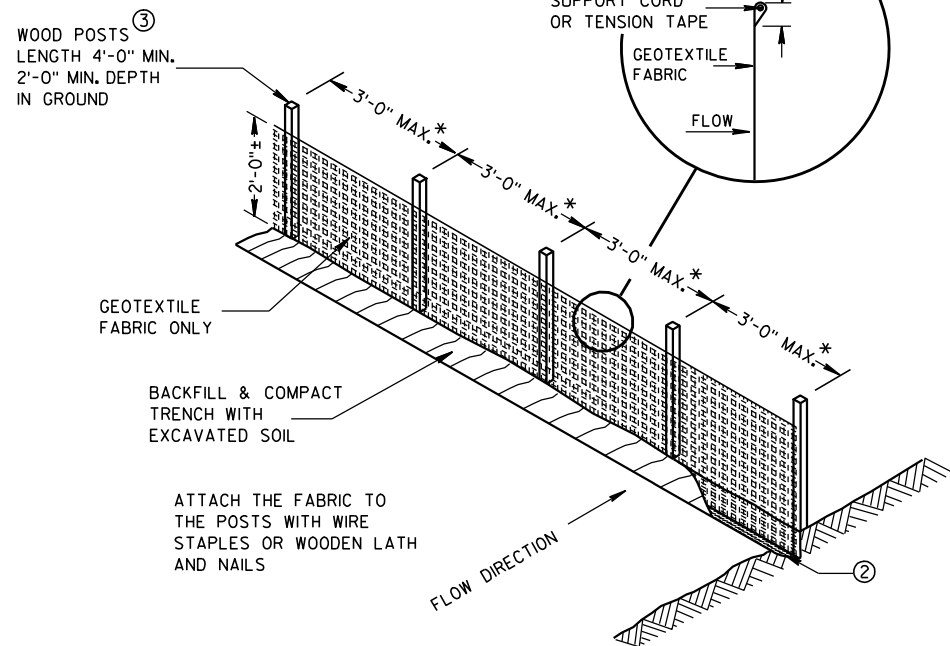
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



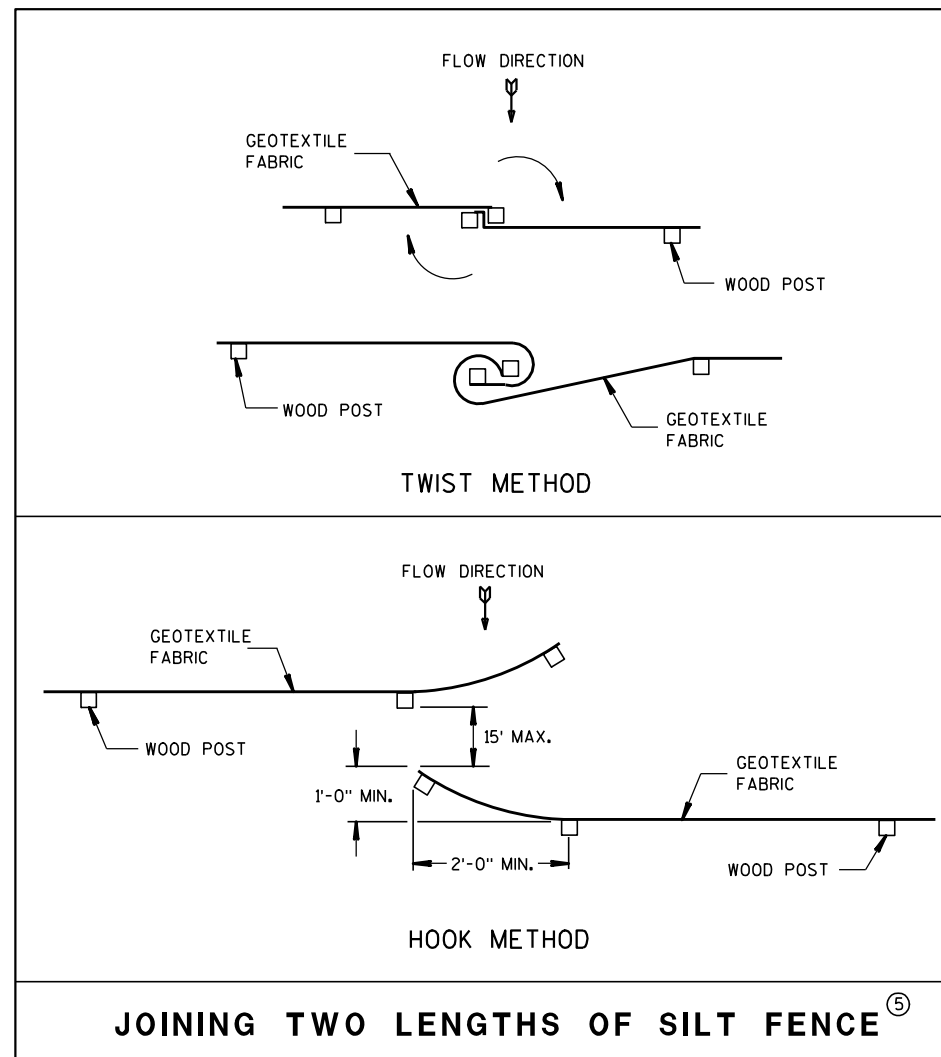
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

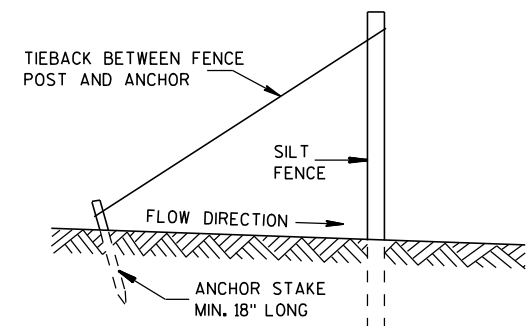


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

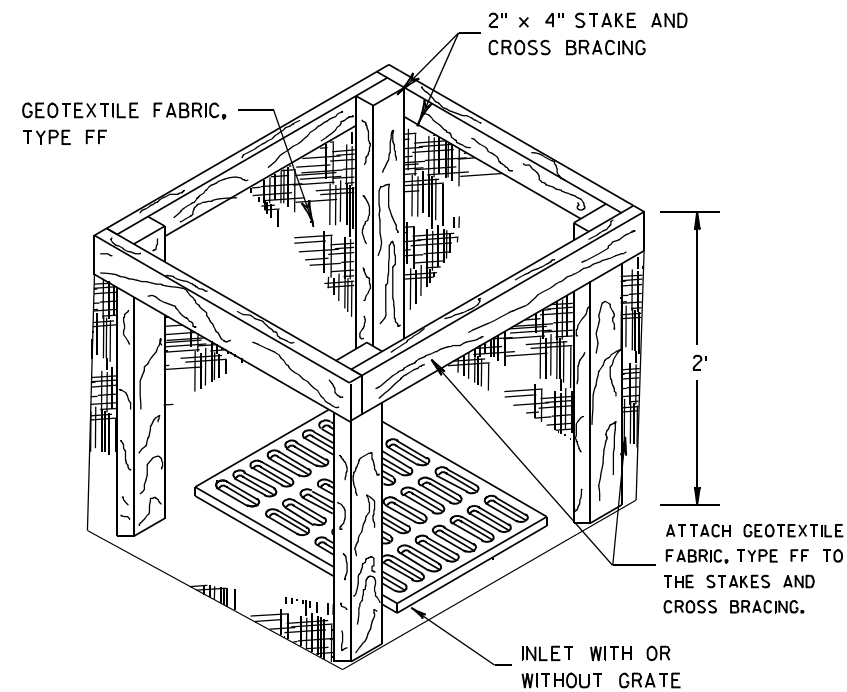
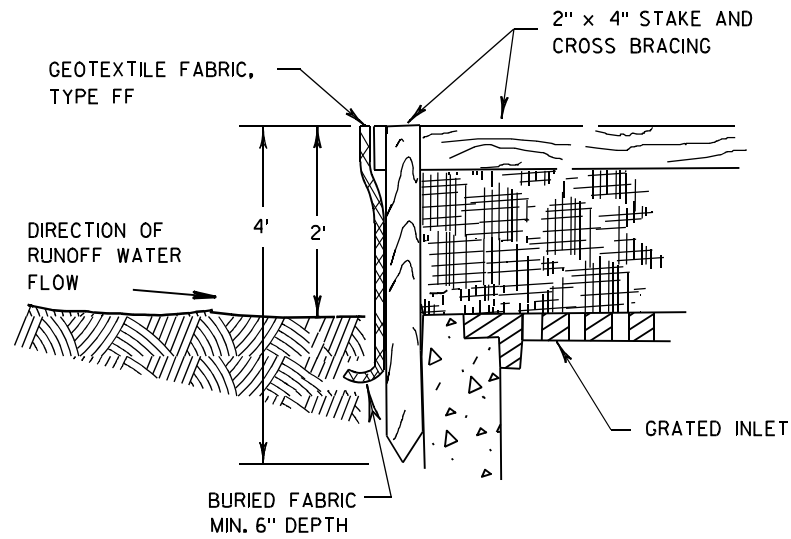


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

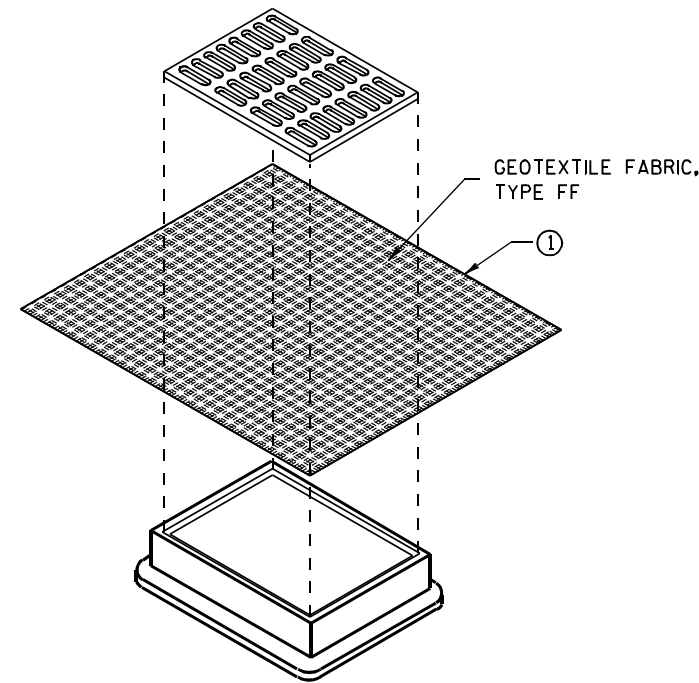
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

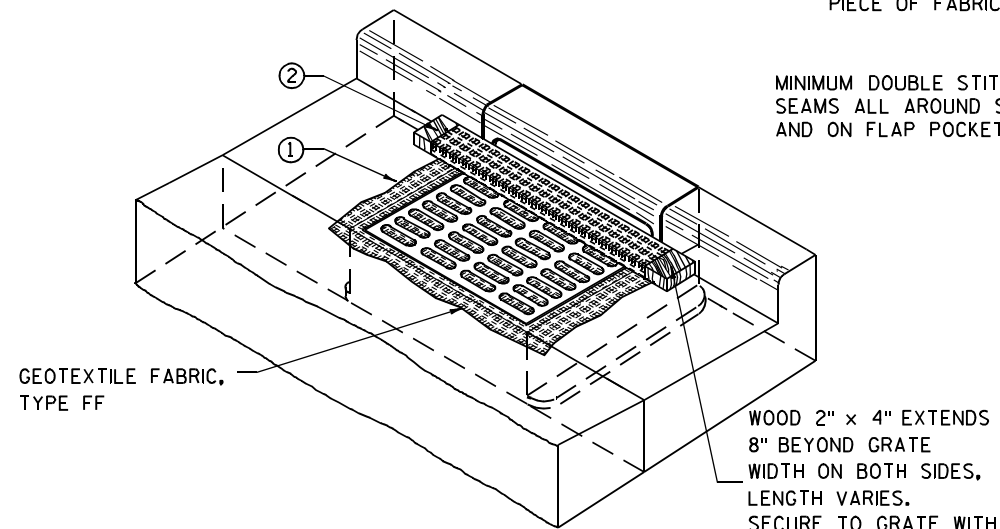
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

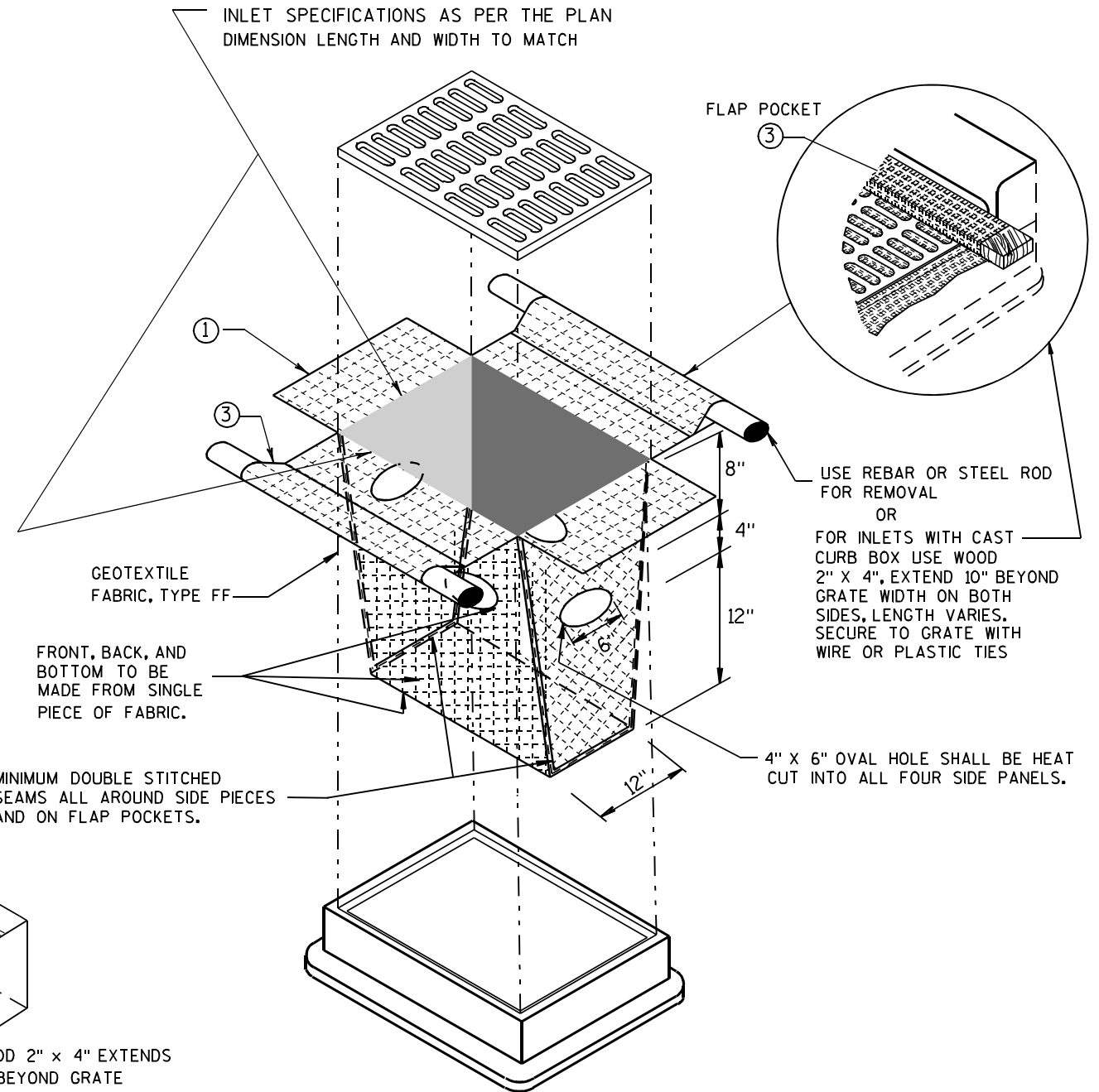
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



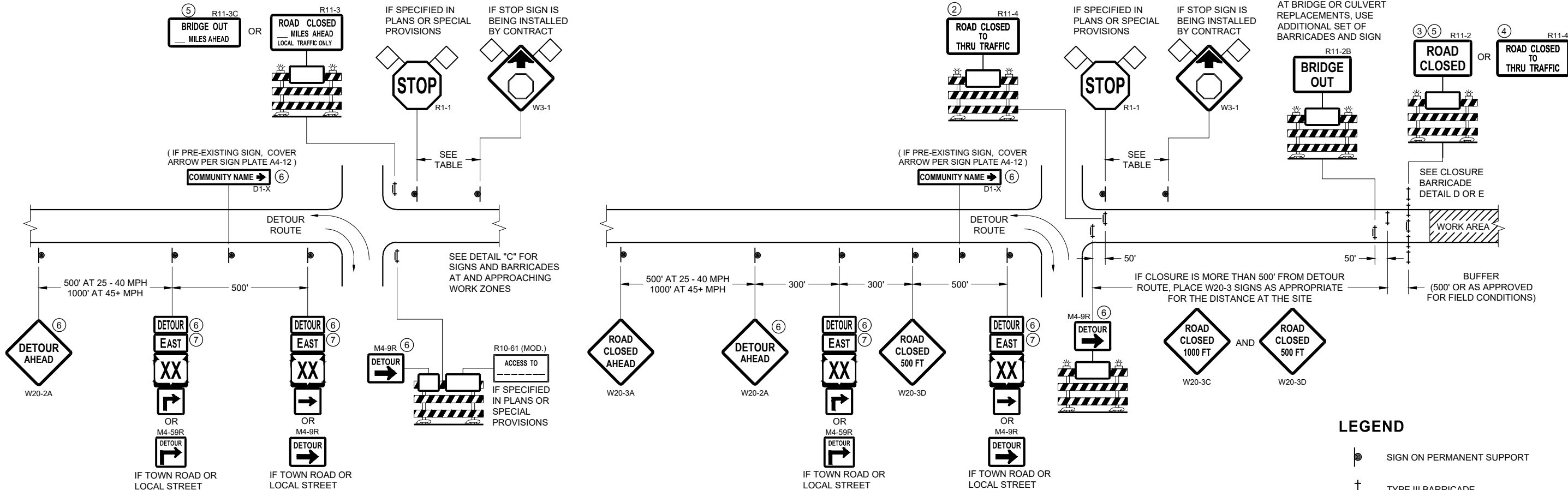
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

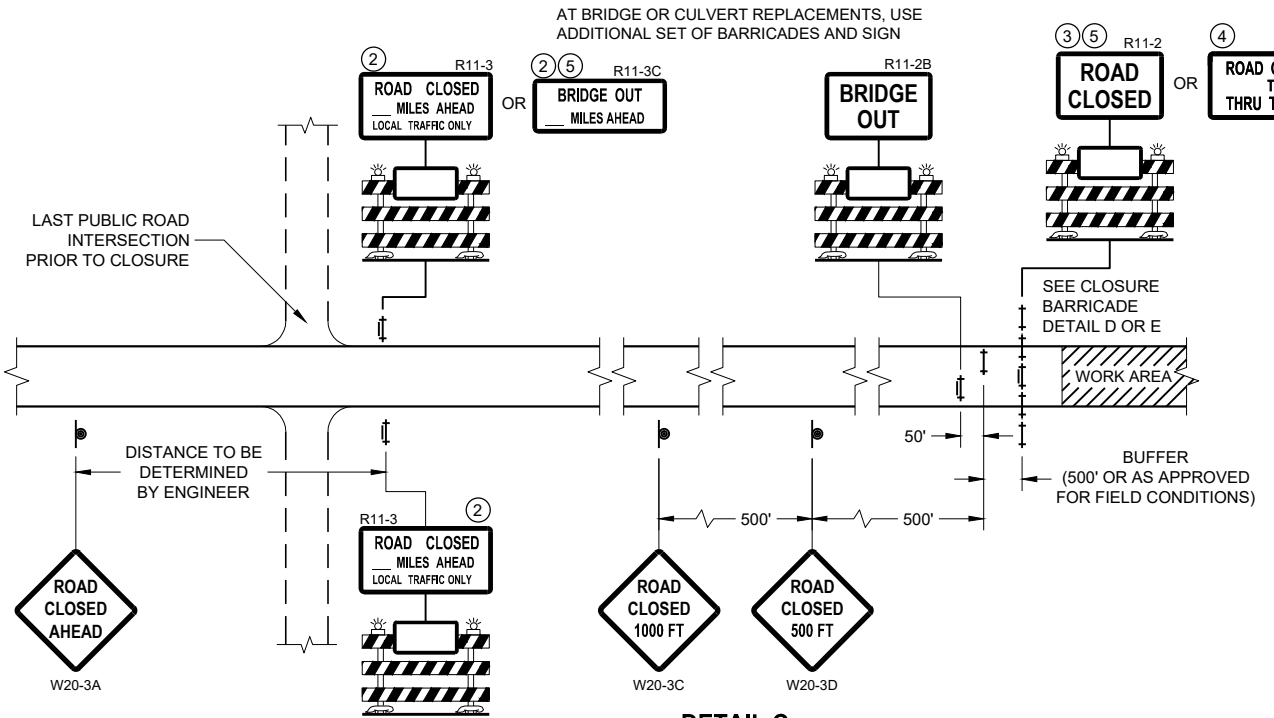
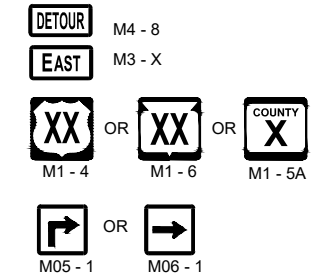


DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE LESS THAN 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TYPE "A" WARNING LIGHT (FLASHING)
 - WORK AREA
 - FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

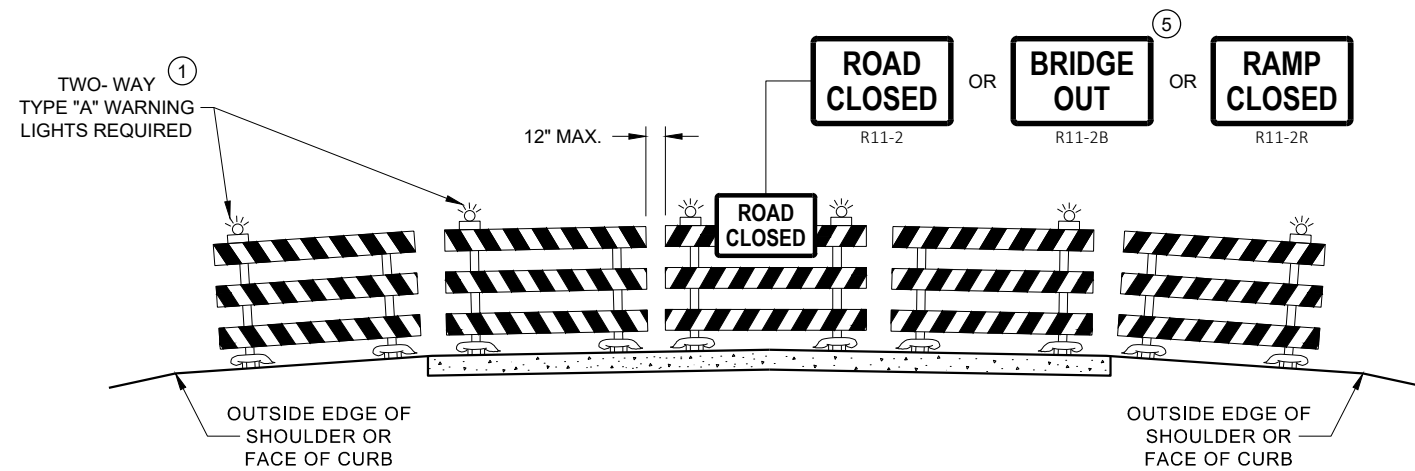
SEE SDD 15C2-SHEET "b"
 FOR GENERAL NOTES
 AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
 FOR MAINLINE CLOSURES**

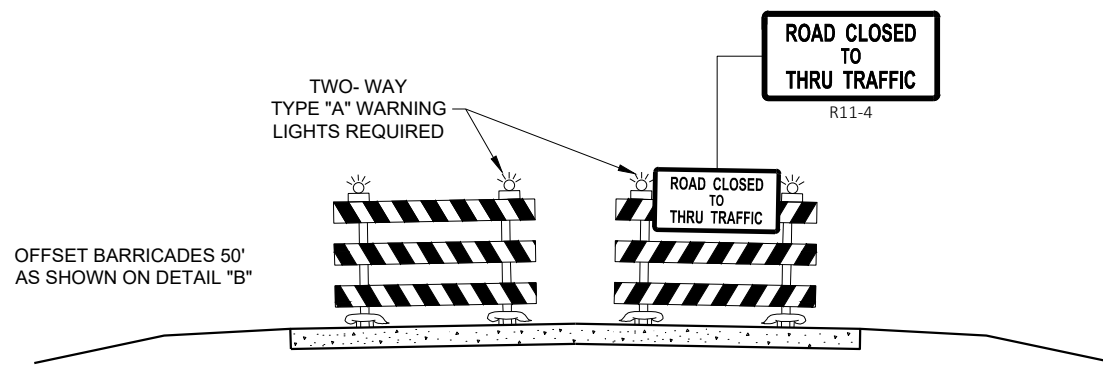
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2018 /S/ Andrew Heidtke
 DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

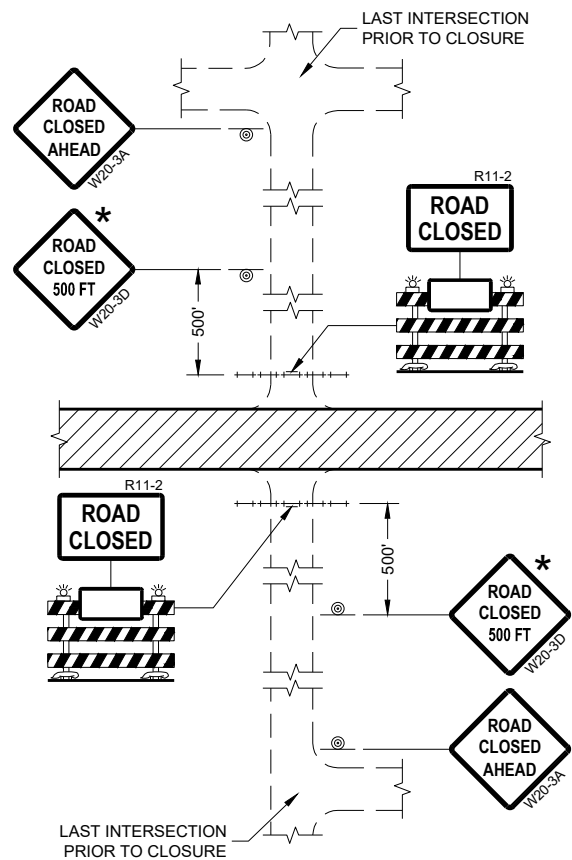
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

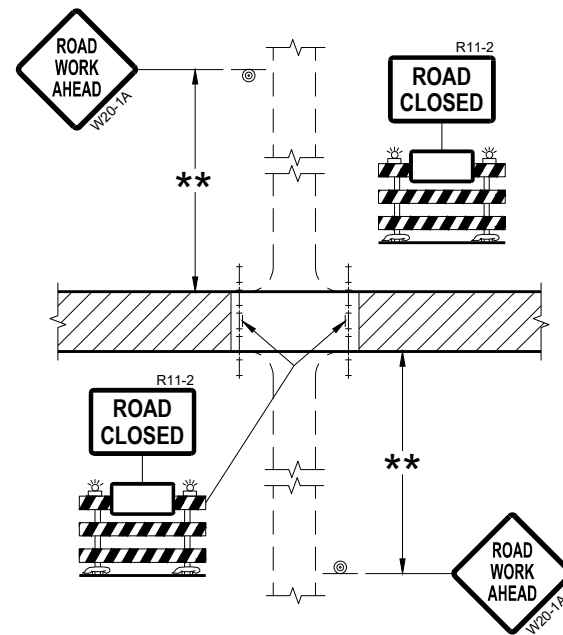
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

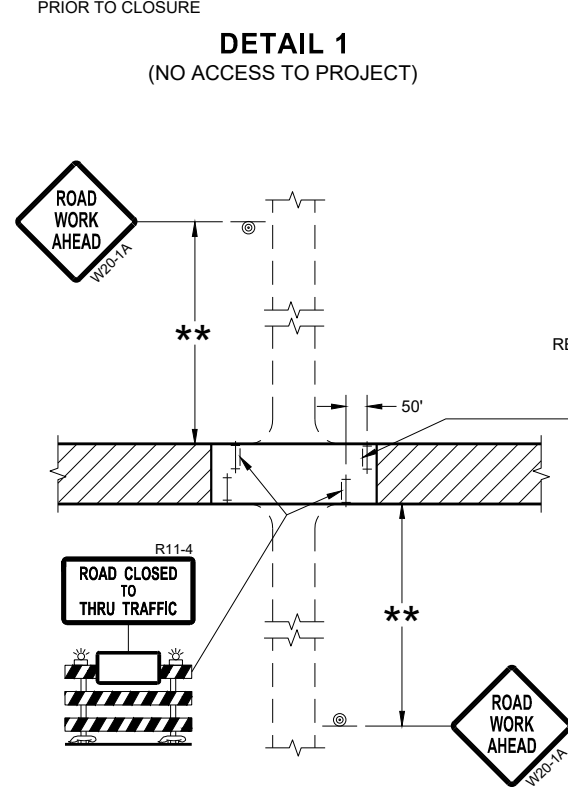
FHWA



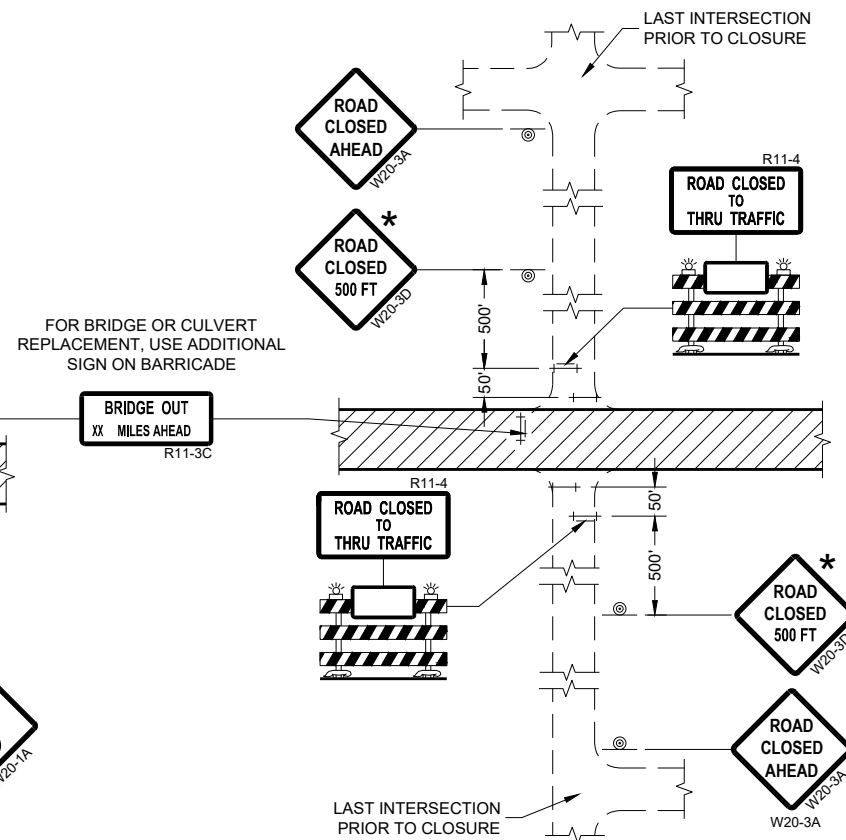
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.

** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

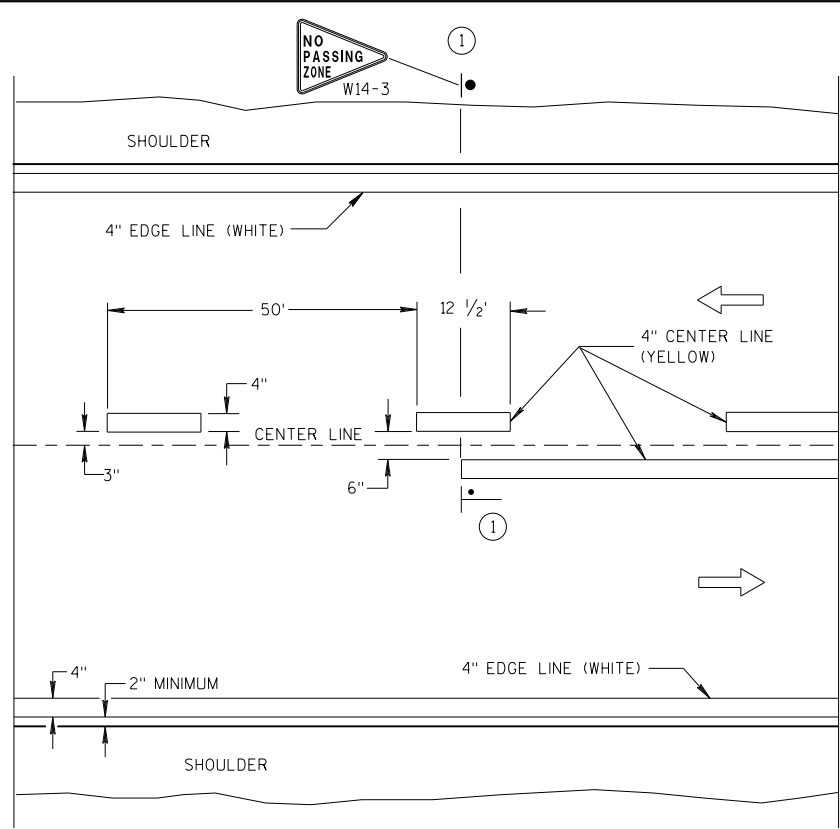
LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

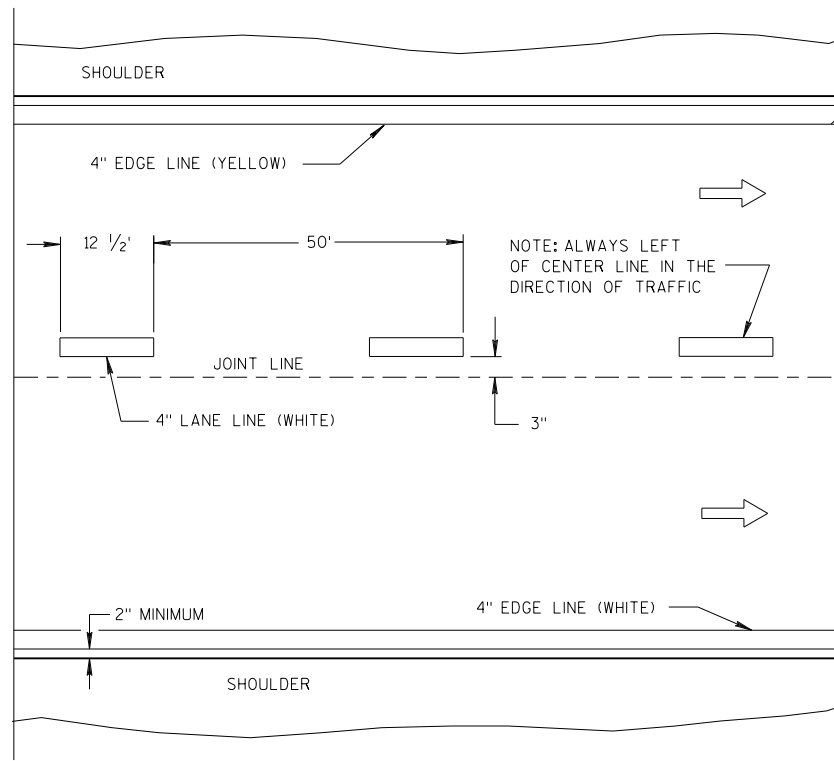
**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

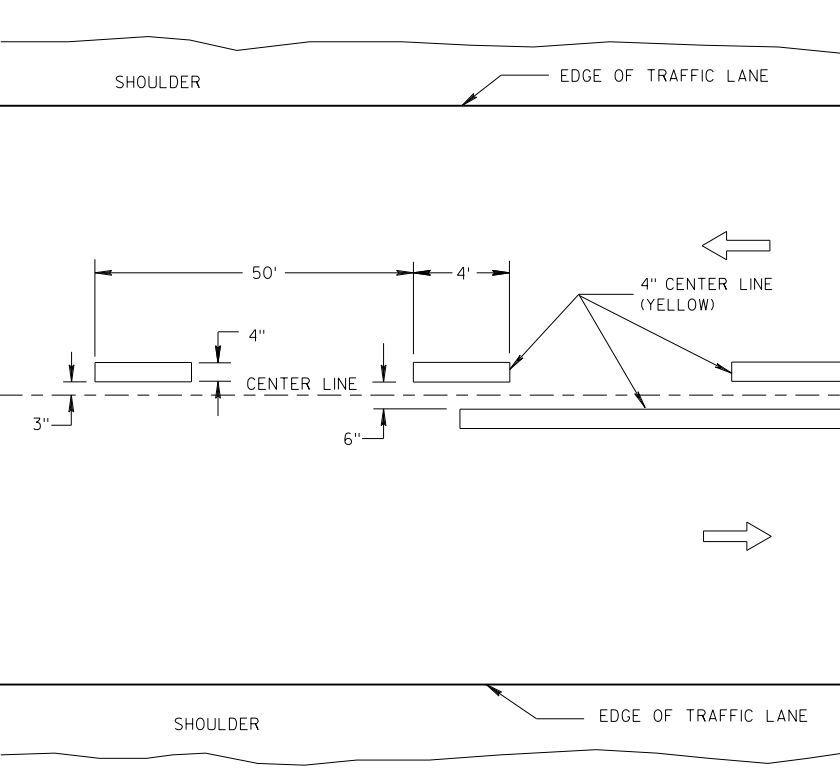


TWO WAY TRAFFIC

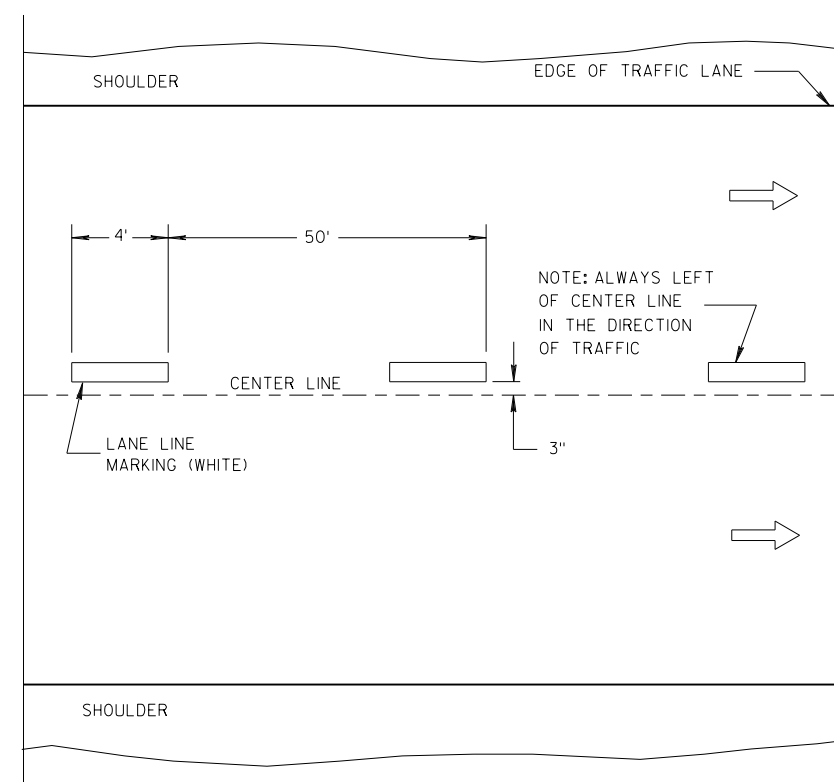


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

LEGEND






- "T" MARKING
- POST MOUNTED SIGN

LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 7/2018 /S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER
FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

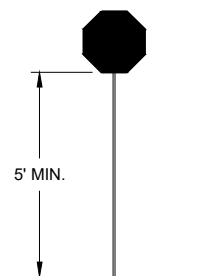
WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
 - ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



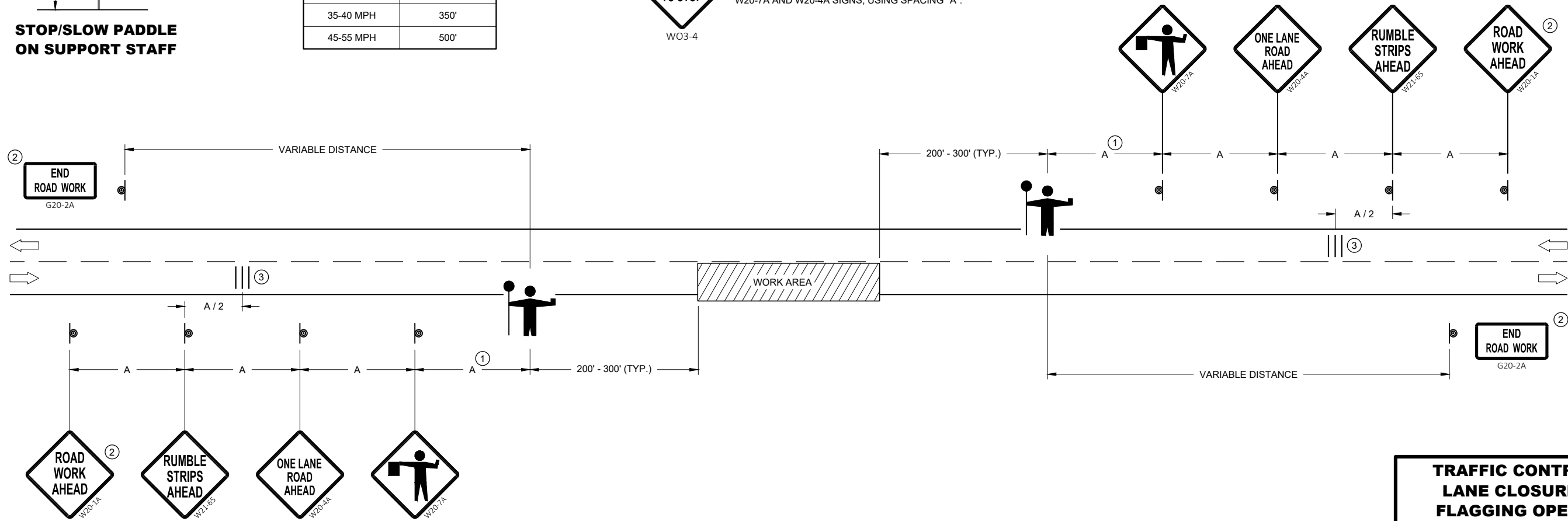
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



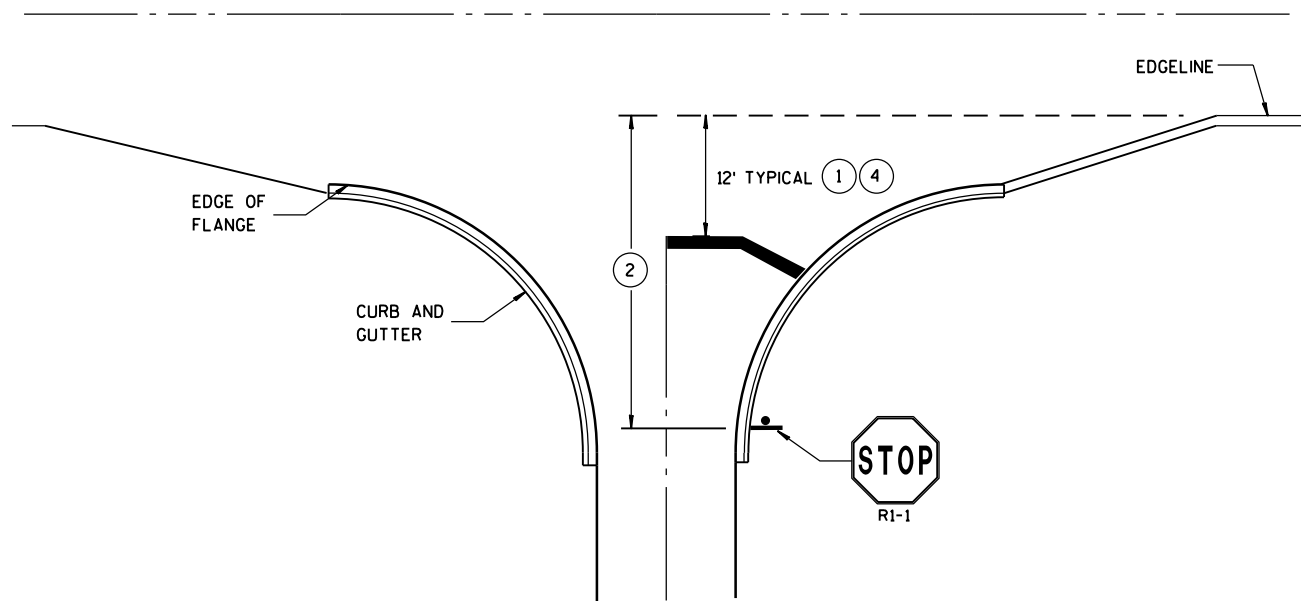
TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

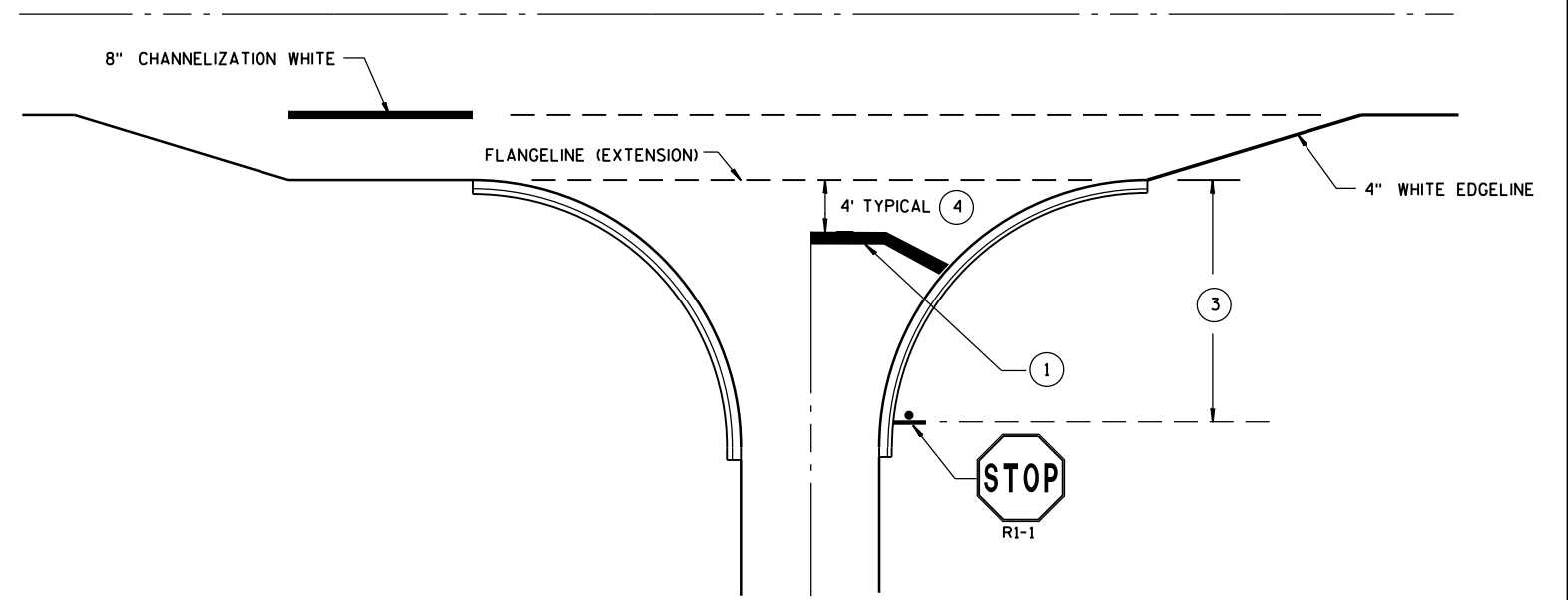
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

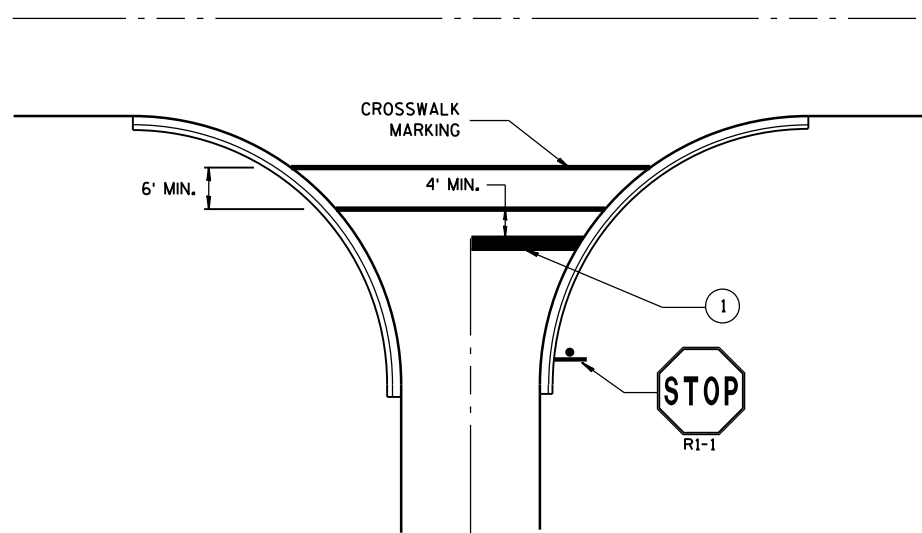
FHWA



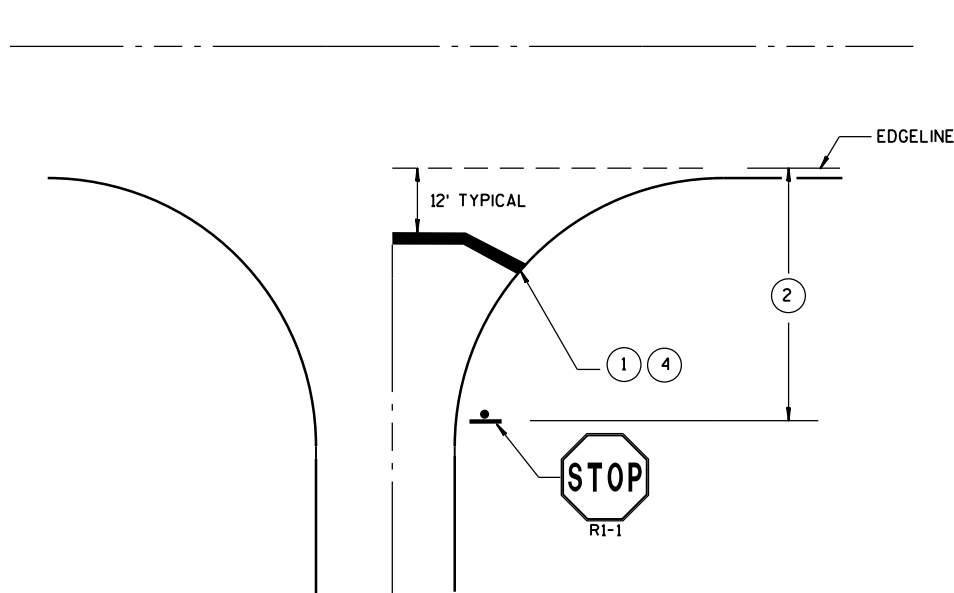
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

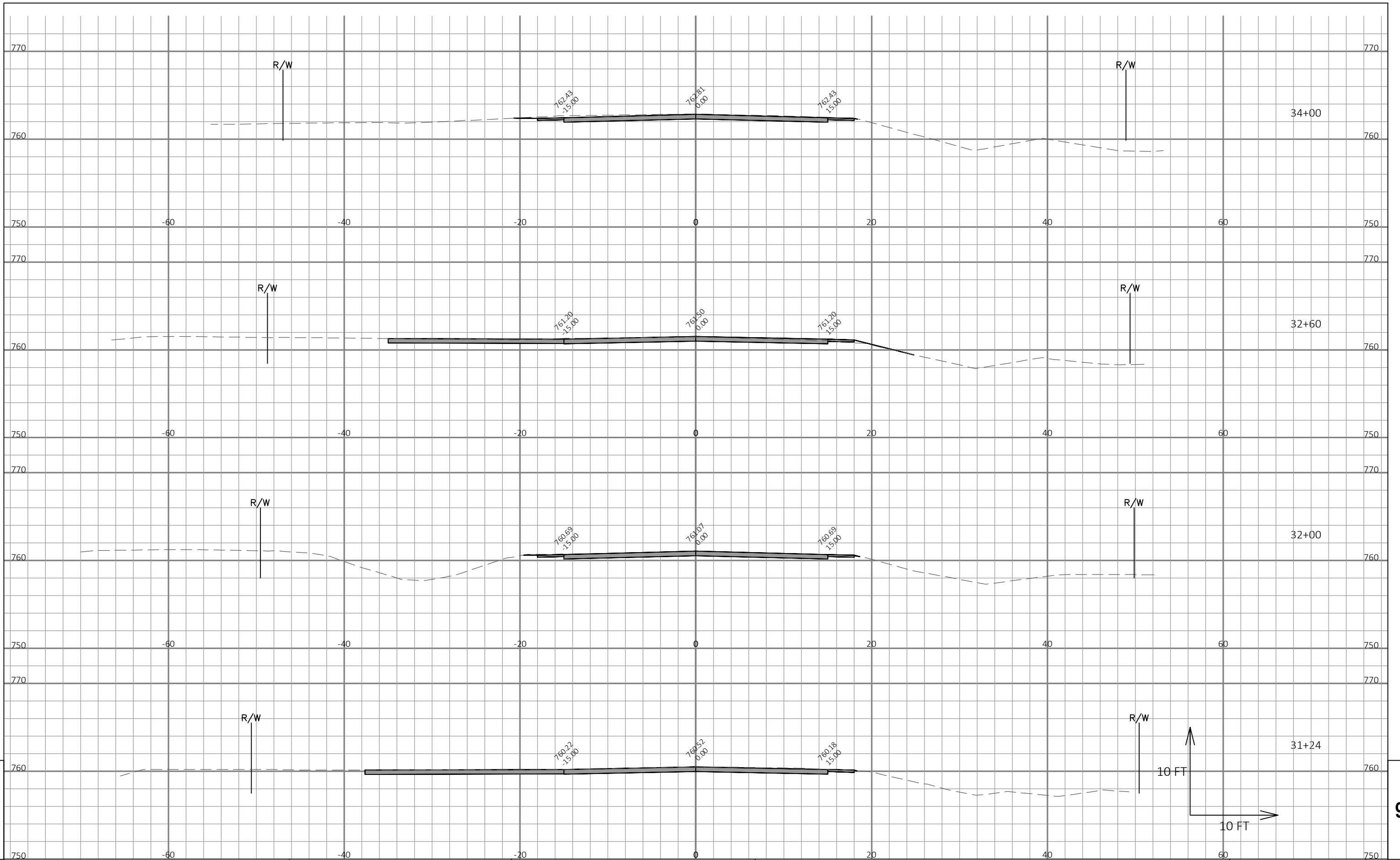
STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

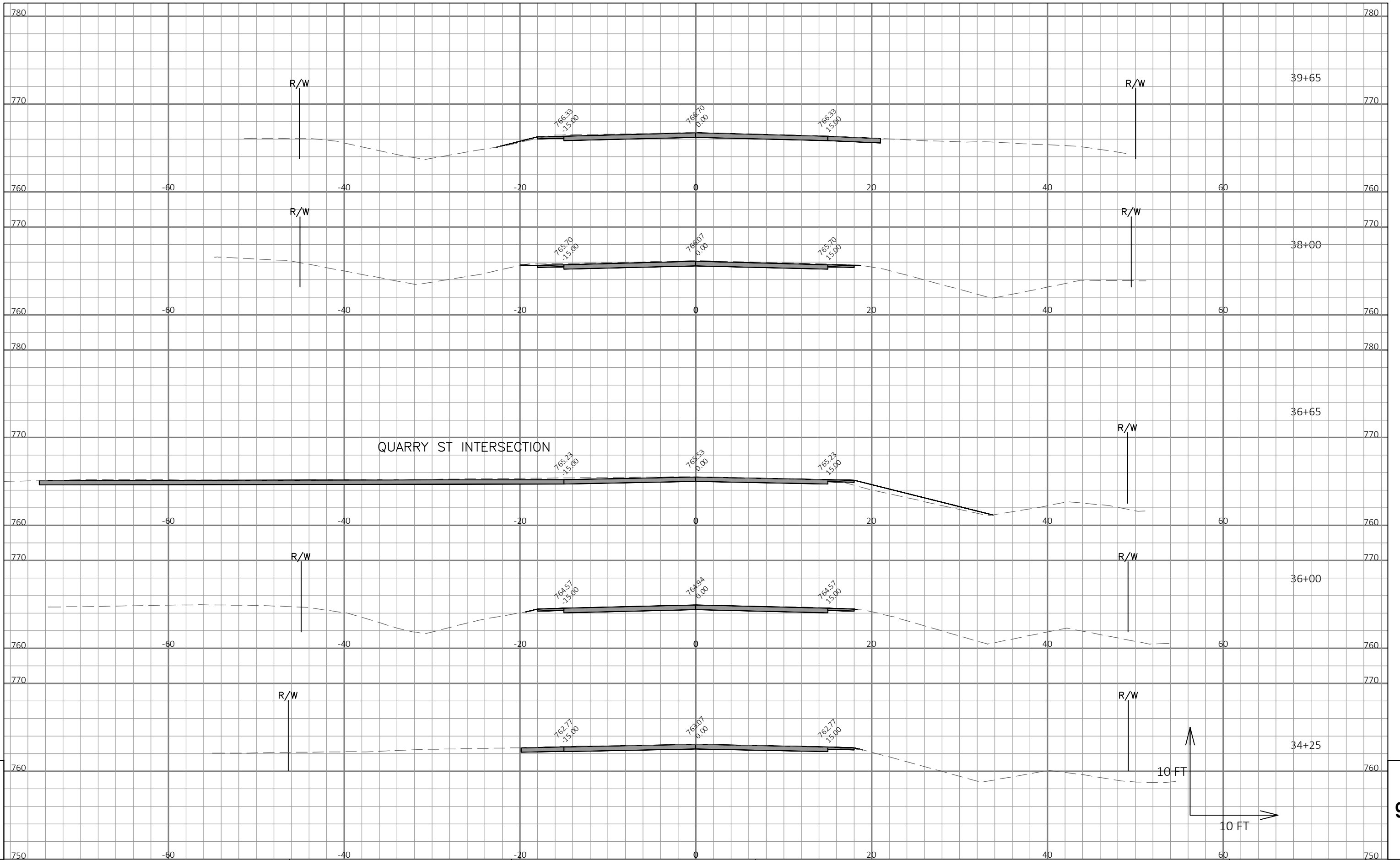
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: Sept., 2017 /S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER
FHWA

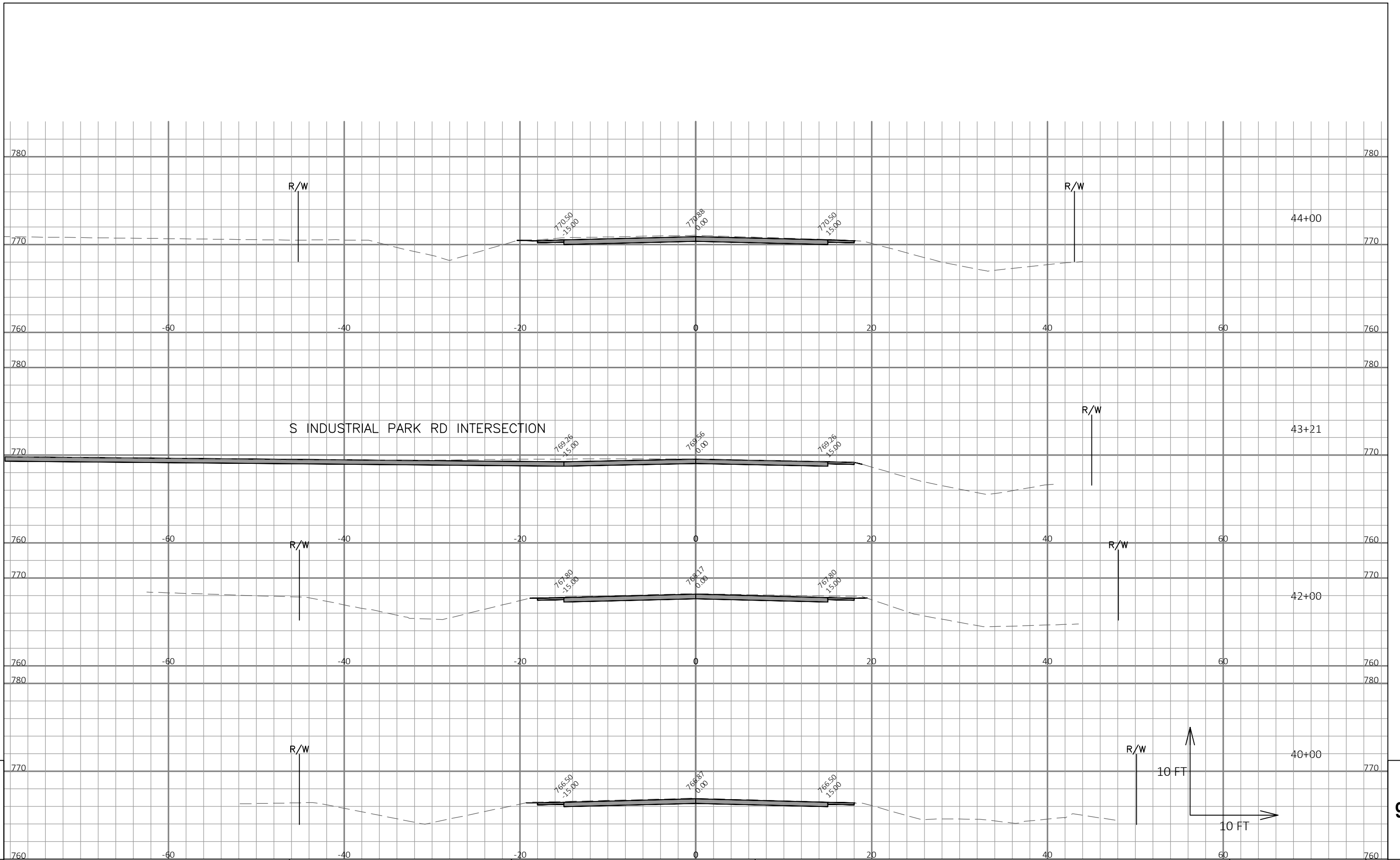


PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE CROSS SECTIONS: RIPON ROAD RESURFACING SHEET 9

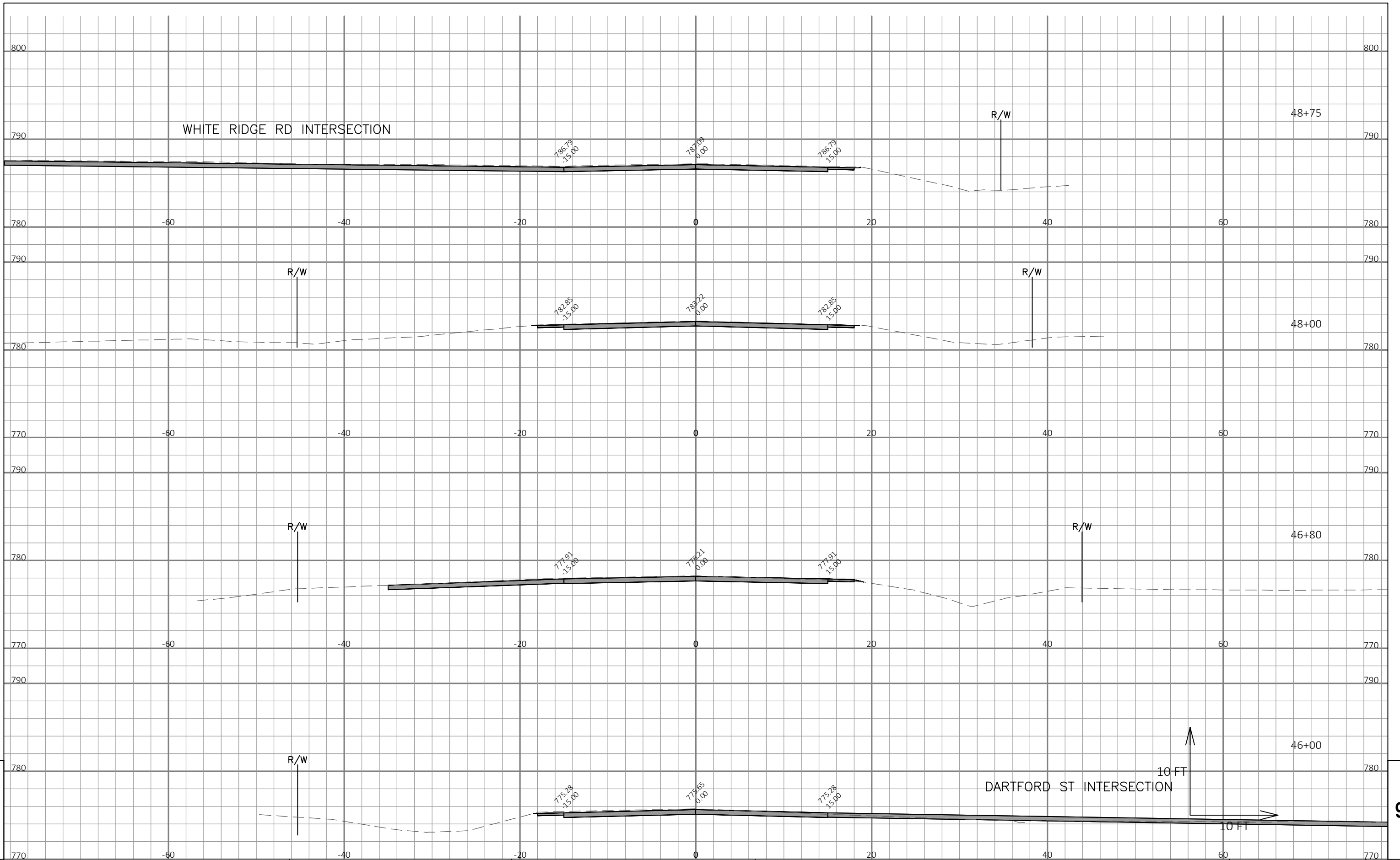
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PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE CROSS SECTIONS: RIPON ROAD RESURFACING SHEET 9



PROJECT NO: 6995-09-71	HWY: CITY OF BERLIN, RIPON ROAD	COUNTY: GREEN LAKE	CROSS SECTIONS: RIPON ROAD RESURFACING	SHEET 9
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PROJECT NO: 6995-09-71

HWY: CITY OF BERLIN, RIPON ROAD

COUNTY: GREEN LAKE

CROSS SECTIONS: RIPON ROAD RESURFACING

SHEET

E

FILE NAME: M:\TOWNS\BERLIN_CITY\RIPON ROAD IMPROVEMENTS\DWG\05-RIPON_RD-6.DWG
LAYOUT NAME - X-Section 1 IN 10 FT Horiz 10 FT Vert (4)

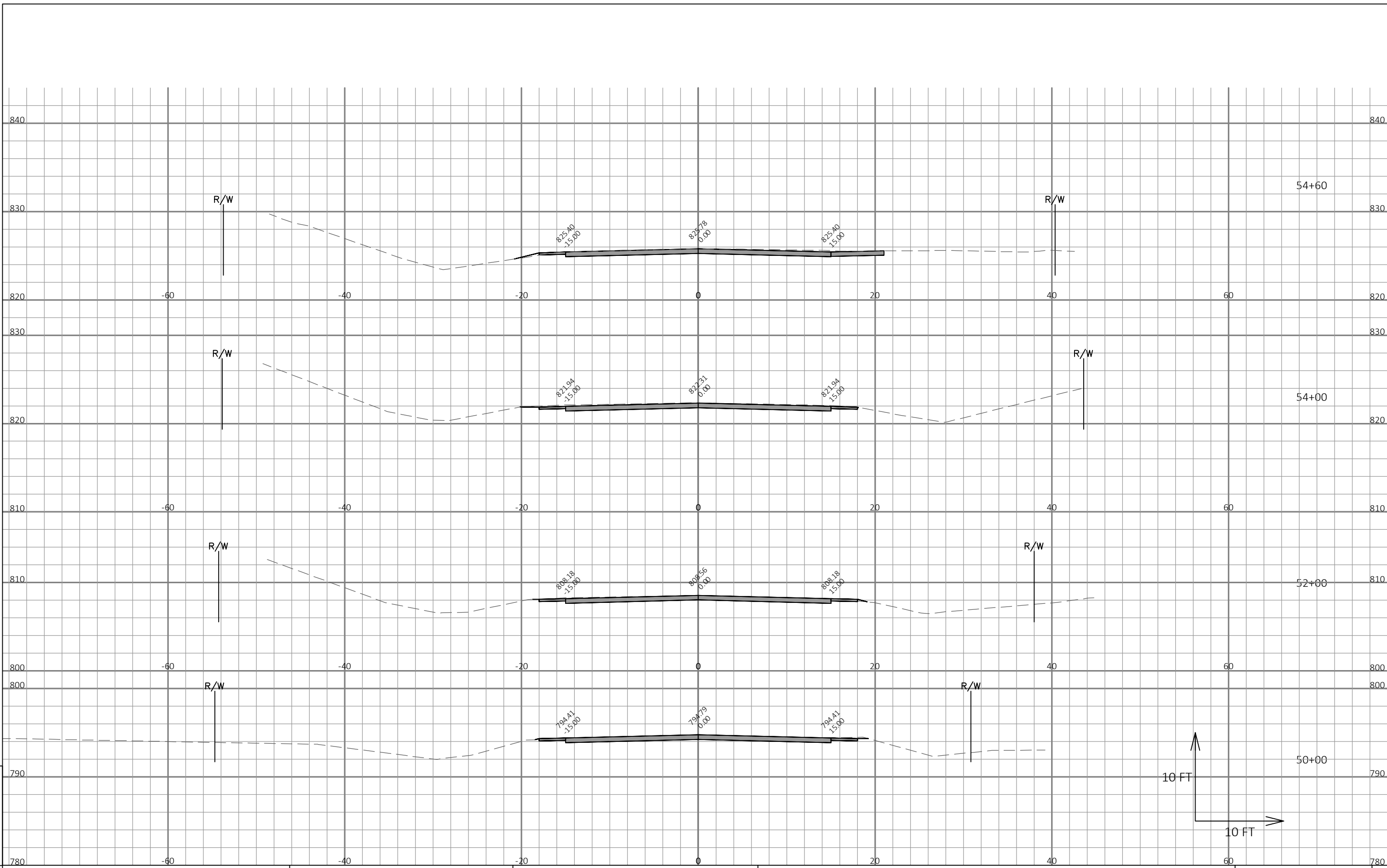
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PLOT BY: ALEX WILLIAMS

PLOT NAME:

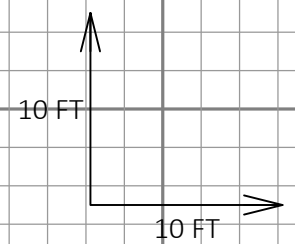
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WISDOT/CADDIS SHEET 49



9

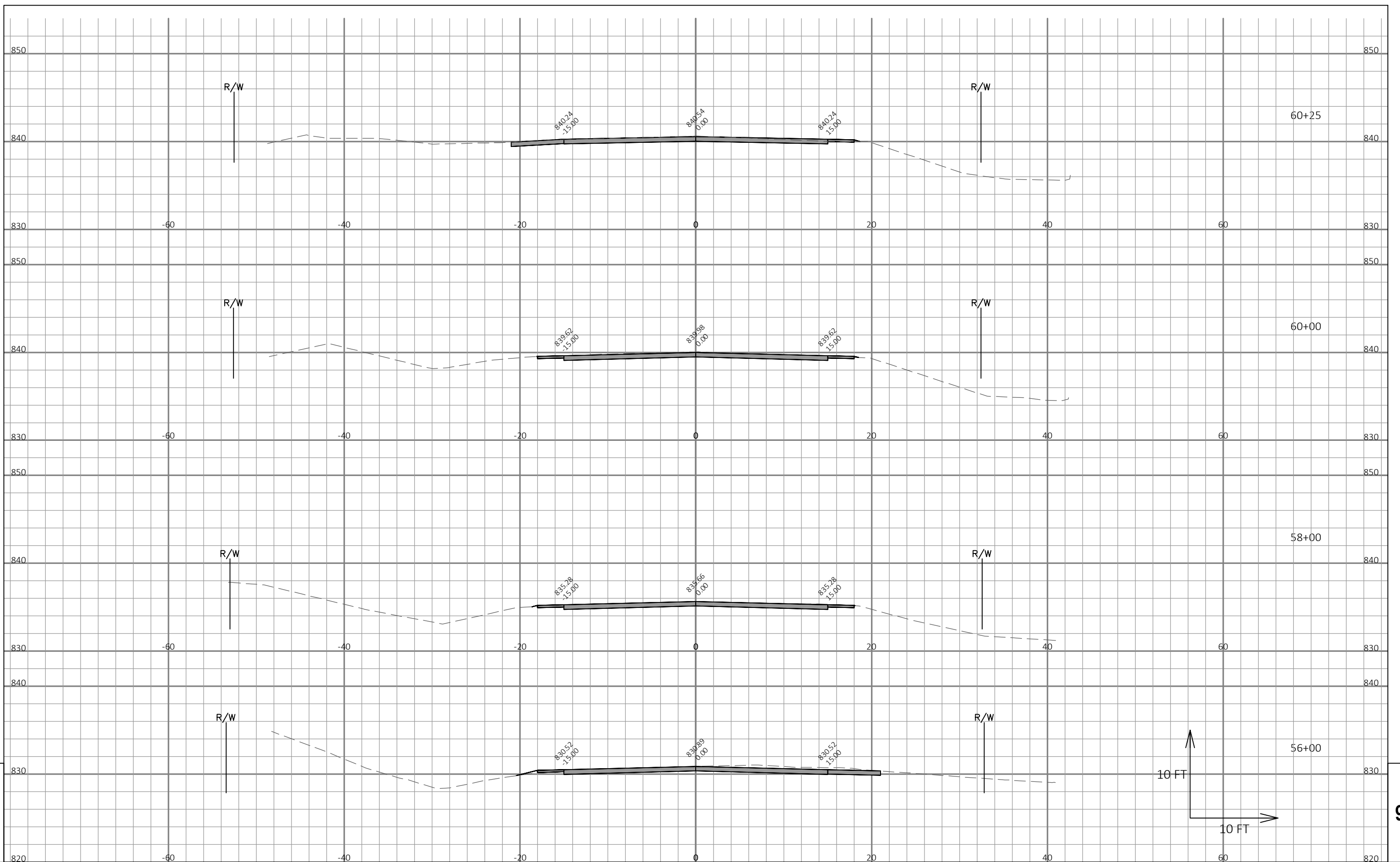
9



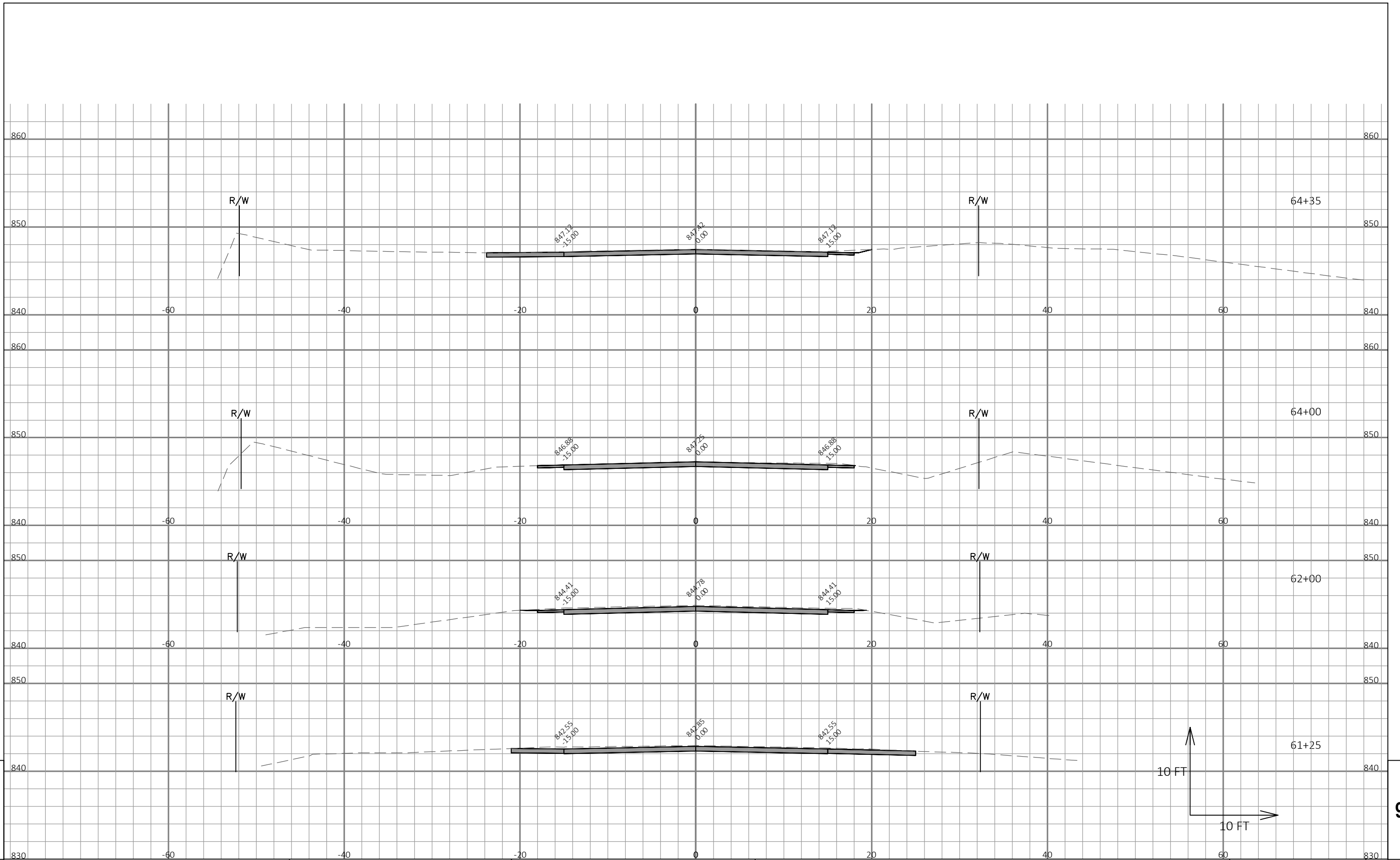
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FILE NAME: M:\TOWNS\BERLIN_CITY\RIPO ROAD IMPROVEMENTS\DWG\05-RIPO RD-6.DWG PLOT DATE: 8/21/2019 2:46 PM PLOT BY: ALEX WILLIAMS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADDS SHEET 49

LAYOUT NAME - X-Section 1 IN 10 FT Horiz 10 FT Vert (5)



PROJECT NO: 6995-09-71 HWY: CITY OF BERLIN, RIPON ROAD COUNTY: GREEN LAKE CROSS SECTIONS: RIPON ROAD RESURFACING SHEET 9



PROJECT NO: 6995-09-71

HWY: CITY OF BERLIN, RIPON ROAD

COUNTY: GREEN LAKE

CROSS SECTIONS: RIPON ROAD RESURFACING

SHEET

E

FILE NAME: M:\TOWNS\BERLIN_CITY\RIPON ROAD IMPROVEMENTS\DWG\05-RIPON_RD-6.DWG
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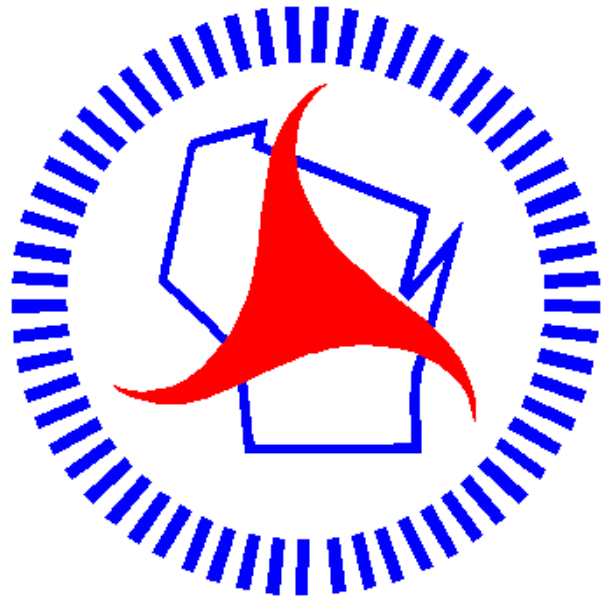
PLOT BY: ALEX WILLIAMS

PLOT NAME:

PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

Notes



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