NOVEMBER 2019

Section No.

Section No.

Section No.

Section No.

TOTAL SHEETS = 82

STATE OF WISCONSIN ORDER OF SHEETS Typical Sections and Details

BEGIN PROJECT

STA 32+50.00 Y=479280.819

X=785922.523

PLAN OF PROPOSED IMPROVEMENT

OSHKOSH AVENUE, CITY OF OSHKOSH

FOX RIVER BRIDGE B-70-0091

STH 21 WINNEBAGO

STATE PROJECT NUMBER 6180-18-71

LAKE **BUTTE DES MORTS**

DEPARTMENT OF TRANSPORTATION

WINNEBAGO

Estimate of Quantities

Plan and Profile Standard Detail Drawings

Structure Plans

Sign Plates

DESIGN DESIGNATION

(2020) = 20,400 VPD (2040) = 23,300 VPD A.A.D.T. D.D. = 59/41 = 6.2% DESIGN SPEED = 35 MPH = N/A

CONVENTIONAL SYMBOLS

CORPORATE LIMITS
PROPERTY LINE
LOT LINE LIMITED HIGHWAY EASEMENT EXISTING RIGHT OF WAY PROPOSED OR NEW R/W LINE
SLOPE INTERCEPT
REFERENCE LINE
EXISTING CULVERT PROPOSED CULVERT (Box or Pipe)
COMBUSTIBLE FLUIDS
MARSH AREA

WOODED OR SHRUB AREA



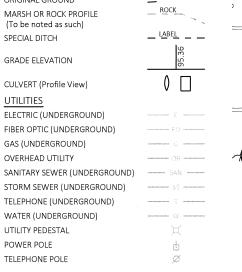
GAS (UNDERGROUND)

WATER (UNDERGROUND)

OVERHEAD LITILITY

UTILITY PEDESTAL

TELEPHONE POLE





OSHKOSH END PROJECT STA 40+00.00 HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, WINNEBAGO COUNTY, NAD83 (91), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID TOTAL NET LENGTH OF CENTERLINE = 0.142 MI

95 South Ploneer Road, Suite 500 • Fond du Lac, WI 54935 (920) 924-5720 • fax (920) 924-5725 MISCONS E-36149 4-26-2019 BENJAMIN L. OITZINGER STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION Surveyor Designer Project Manage Regional Examine Regional Supervisor William R. Bertrand

(Signature)

GREMMER & ASSOCIATES, INC. CONSULTING ENGINEERS

ORIGINAL PLANS PREPARED BY

FEDERAL PROJECT

WISC 2019689

STATE PROJECT

6180-18-71

DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN

THE CONTRACTOR SHALL NOTICY DIGGERS HOTLINE AND AFFECTED LITHTIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

A VERTICAL SAW CUT SHALL BE MADE THROUGH EXISTING DRIVEWAYS, SIDEWALKS AND PAVEMENTS AT THE REMOVAL LIMITS.

SAWCUT LOCATIONS SHOWN ON THE PLANS ARE SUBJECT TO ADJUSTMENT BY THE ENGINEER IN THE FIELD.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

TOPSOIL, FERTILIZER, SEED AND EROSION MAT AS SHOWN IN PLANS OR AS DIRECTED BY THE ENGINEER SHALL BE PLACED ON ALL DISTURBED AREAS, EXCLUSIVE OF THE AREA OCCUPIED BY THE NEW PAVEMENTS, SIDEWALKS, ENTRANCES. AND RELATED STRUCTURES.

EROSION CONTROL ITEMS SHOWN ARE APPROXIMATE, THE EXACT LOCATION SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THAT THE MEASURE IS NO LONGER NECESSARY. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING EROSION CONTROL MEASURE AS DIRECTED BY THE ENGINEER.

THE FIRST SET OF INLETS DOWNSTREAM OF THE PROJECT SHALL RECEIVE

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING, OR PARKING LANF

POINT#

FILE NAME

ABBREVIATIONS

STA

SY TOC

TYP

CONTROL POINT TABLE

DESCRIPTION

MAG NAIL IN CONCRETE JOINT, CENTERLINE OF CONGRESS AVENUE, JUST WEST OF SUMMIT AVENUE

MAG NAIL IN CONCRETE JOINT, CENTERLINE OF OSHKOSH AVENUE, 150' WEST OF FOX RIVER

MAG NAIL IN SIDEWALK JOINT, NORTH SIDE OF CONGRESS AVENUE, 150' EAST OF FOX RIVER

AGG AH ASPH AGGREGATE AHEAD ASPHALT BK BAD BASE AGGREGATE DENSE BM CC BENCH MARK CENTER OF CURVATURE COMMERCIAL ENTRANCE CURB AND GUTTER CE C&G C/L CONC CENTER OR CONSTRUCTION LINE CONCRETE SURFACE DRAIN CSD CY ELEV CUBIC YARD ELEVATION FE HMA FIELD ENTRANCE HOT MIX ASPHALT LEFT LT MAX MAXIMUM MIN M/L MINIMUM MATCHLINE PAVT PAVEMENT PRIVATE ENTRANCE POINT OF INTERSECTION REQ'D REQUIRED SDD SEG SF STANDARD DETAIL DRAWING

SEGMENT

STATION

TYPICAL

SQUARE FOOT

SQUARE YARD

TOP OF CURB

VELOCITY OR DESIGN SPEED

ORDER OF SECTION 2 SHEETS

GENERAL NOTES TYPICAL SECTIONS TRAFFIC CONTROL

DETOUR

UTILITIES

700 N ADAMS ST P.O. BOX 19001

GREEN BAY, WI 54307

PHONE: (920) 433-1703

ATTN: LORI BUTRY

WISCONSIN PUBLIC SERVICE CORPORATION 700 N ADAMS ST P.O. BOX 19001

GREEN BAY, WI 54307 ATTN: LORI BUTRY PHONE: (920) 433-1703 EMAIL: labutry@integrysgroup.com

COMMUNICATIONS

WISCONSIN PUBLIC SERVICE CORPORATION 3520 DESTINATION DRIVE APPLETON, WI 54915 ATTN: VINCE ALBIN PHONE: (920) 831-9249 MOBILE: (920) 378-0444 EMAIL: labutry@integrysgroup.com EMAIL: vince.albin@charter.com

ELECTRIC TRANSMISSION

ATC MANAGEMENT INC 801 O'KEEFE RD P.O. BOX 6113 DE PERE, WI 54155 ATTN: MIKE OLSEN PHONE: (920) 338-6582 EMAIL: molsen@atcllc.com

COMMUNICATIONS

AT&T WISCONSIN 70 FAST DIVISION STREET FOND DU LAC, WI 54935 ATTN: CHUCK BARTELT PHONE: (920) 929-1013 MOBILE: (920) 410-5104 EMAIL: cb1461@att.com

CHARTER COMMUNICATIONS

COMMUNICATIONS

WISCONSIN DEPARTMENT OF TRANSPORTATION 944 VANDERPERREN WAY GREEN BAY, WI 54304 ATTN: JASON LAHM PHONE: (920) 360-2374 EMAIL: jason.lahm@dot.wi.gov

WATER

CITY OF OSHKOSH 215 CHURCH AVENUE P.O. BOX 1130 OSHKOSH, WI 54903-1130 ATTN: JUSTIN GIERECH PHONE: (920) 236-5065 EMAIL: jgierech@ci.oshkosh.wi.us

SEWER

CITY OF OSHKOSH 215 CHURCH AVENUE P.O. BOX 1130 OSHKOSH, WI 54903-1130 ATTN: JUSTIN GIERECH PHONE: (920) 236-5065 EMAIL: jgierech@ci.oshkosh.wi.us

ELECTRIC

CITY OF OSHKOSH 926 DEMPSEY TRAIL OSHKOSH, WI 54902 ATTN: DAN KUSSMANN PHONE: (920) 232-5350 EMAIL: dkussmann@ci.oshkosh.wi.us



EASTING

785827.6067

786686.6674

786971.8839

NORTHING

479287.5836

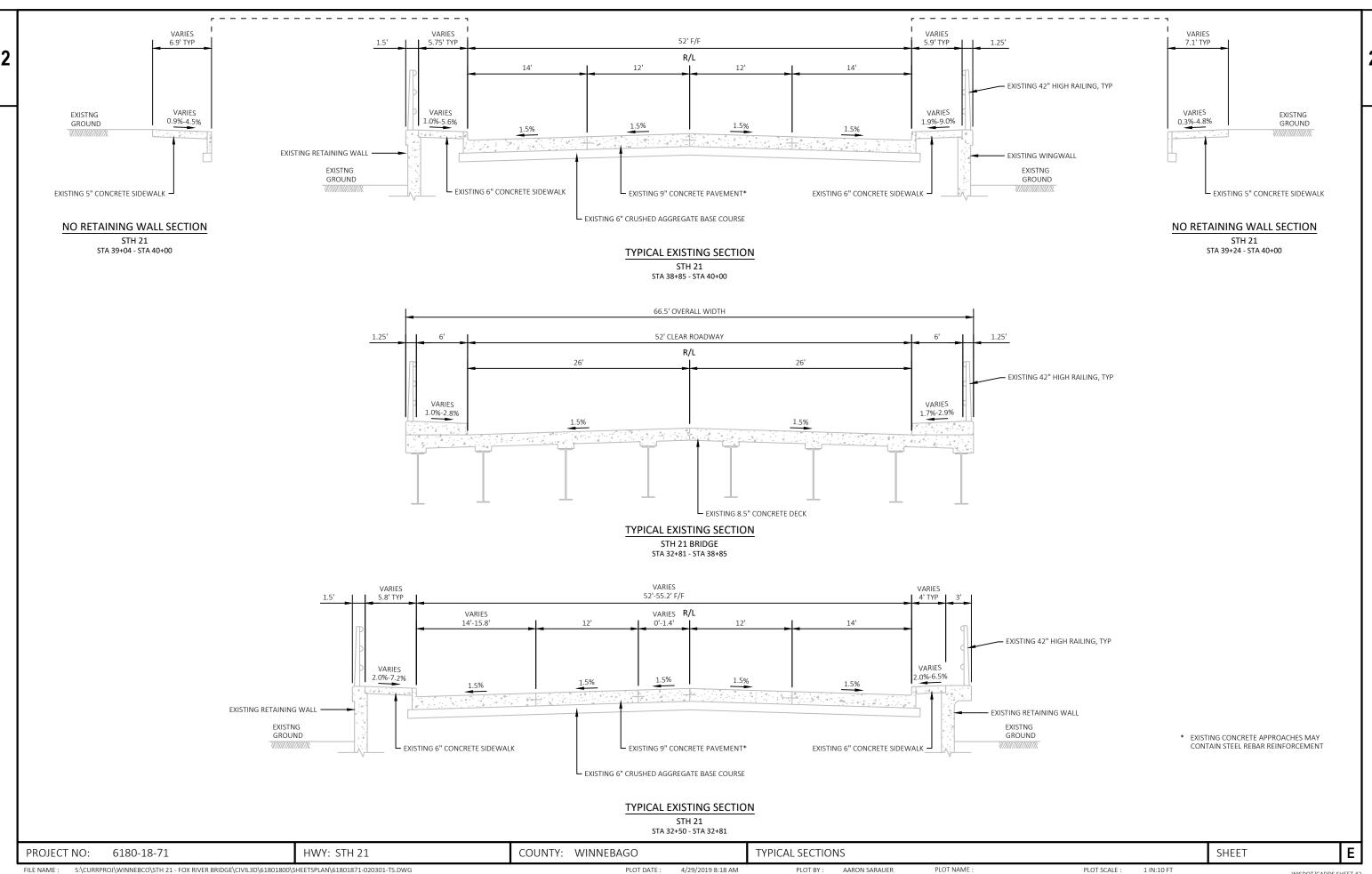
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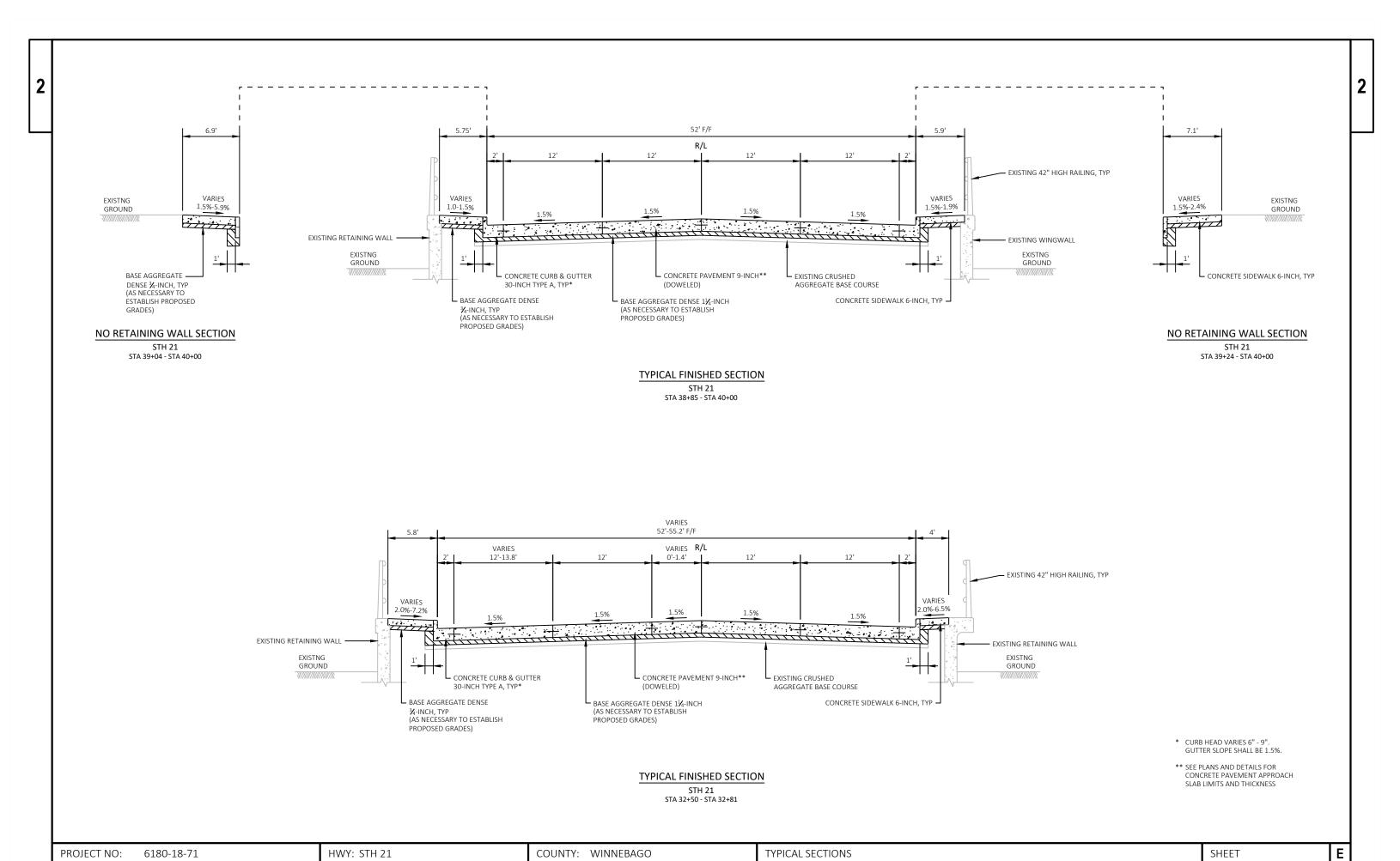
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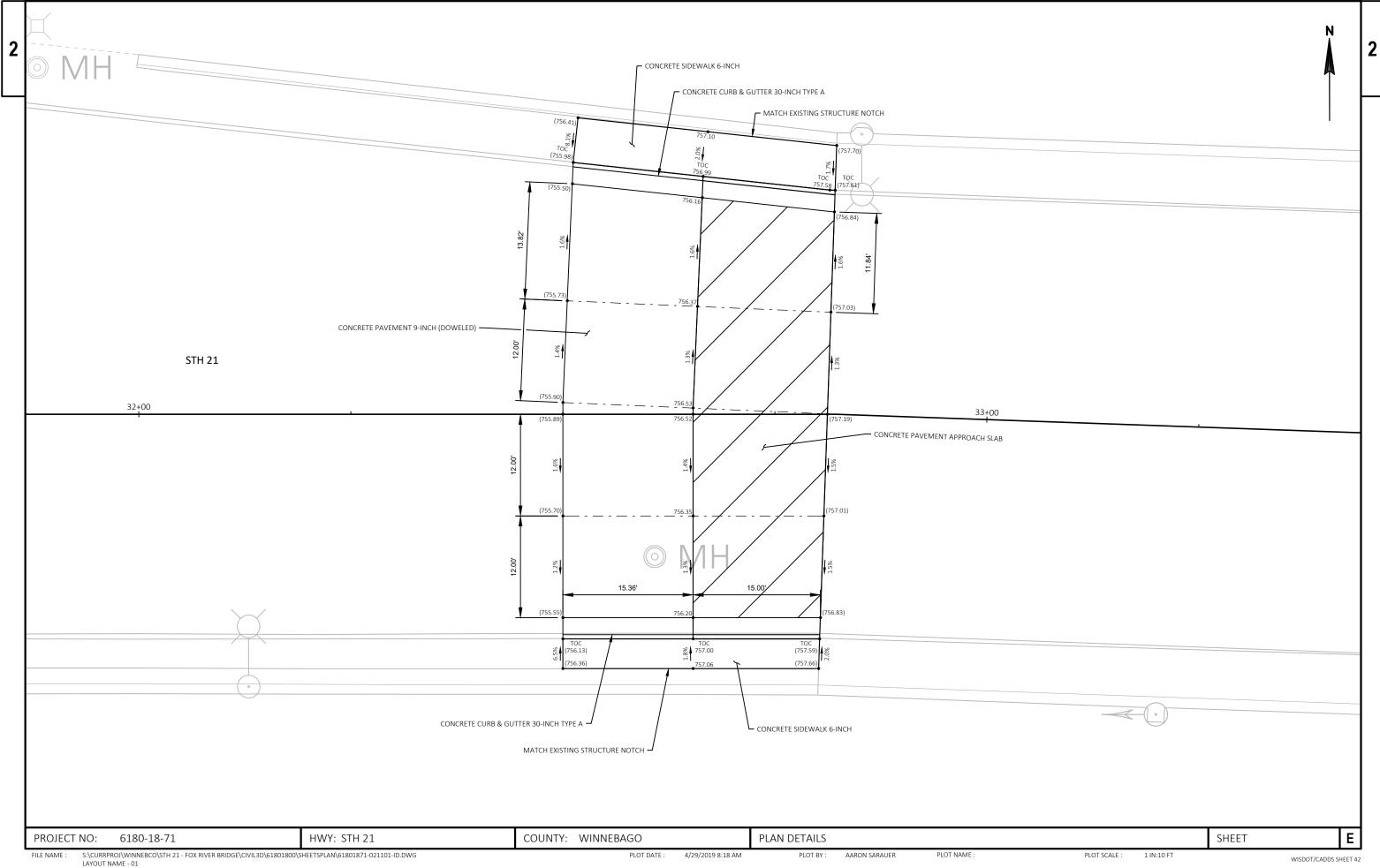
DNR AREA LIAISON

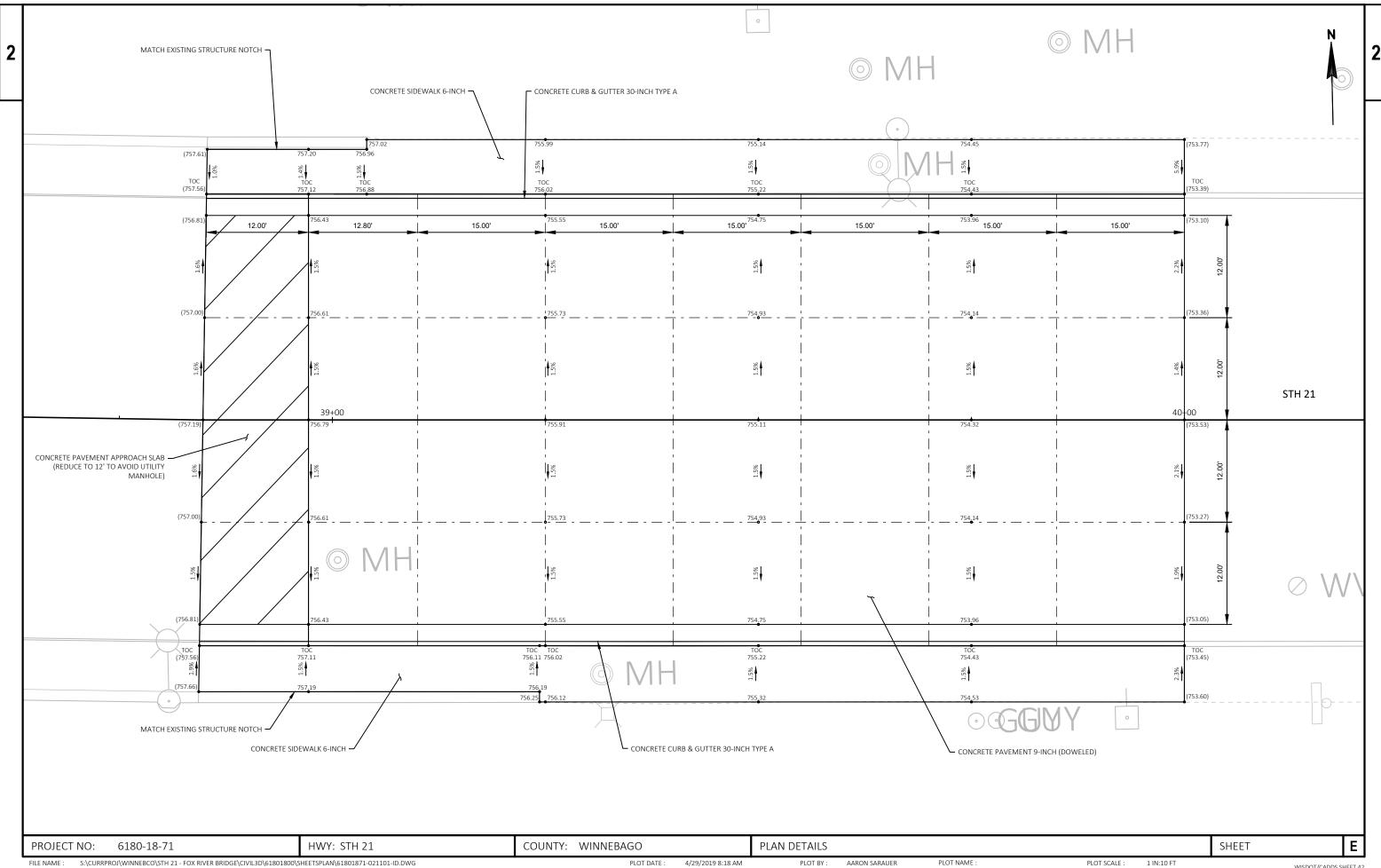
WISCONSIN DEPT. OF NATURAL RESOURCES WDNR NORTHEAST REGIONAL HQ 2984 SHAWANO AVENUE GREEN BAY, WI 54313-6727 ATTN: JAY SCHIEFELBEIN PHONE: (920) 360-3784 EMAIL: JEREMIAH.SCHIEFELBEIN@WISCONSIN.GOV

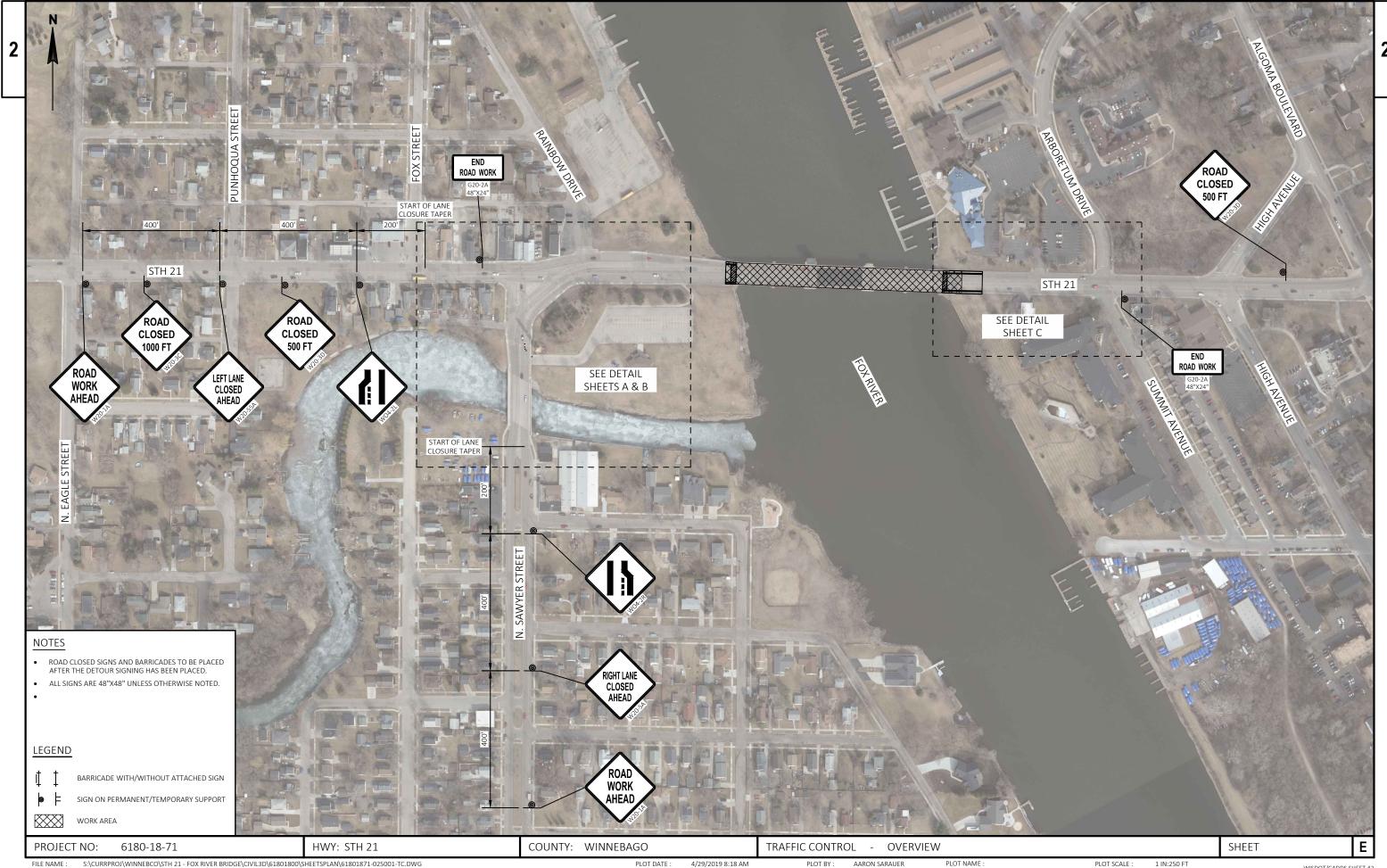
Ε PROJECT NO: 6180-18-71 HWY: STH 21 COUNTY: WINNEBAGO **GENERAL NOTES SHEET**

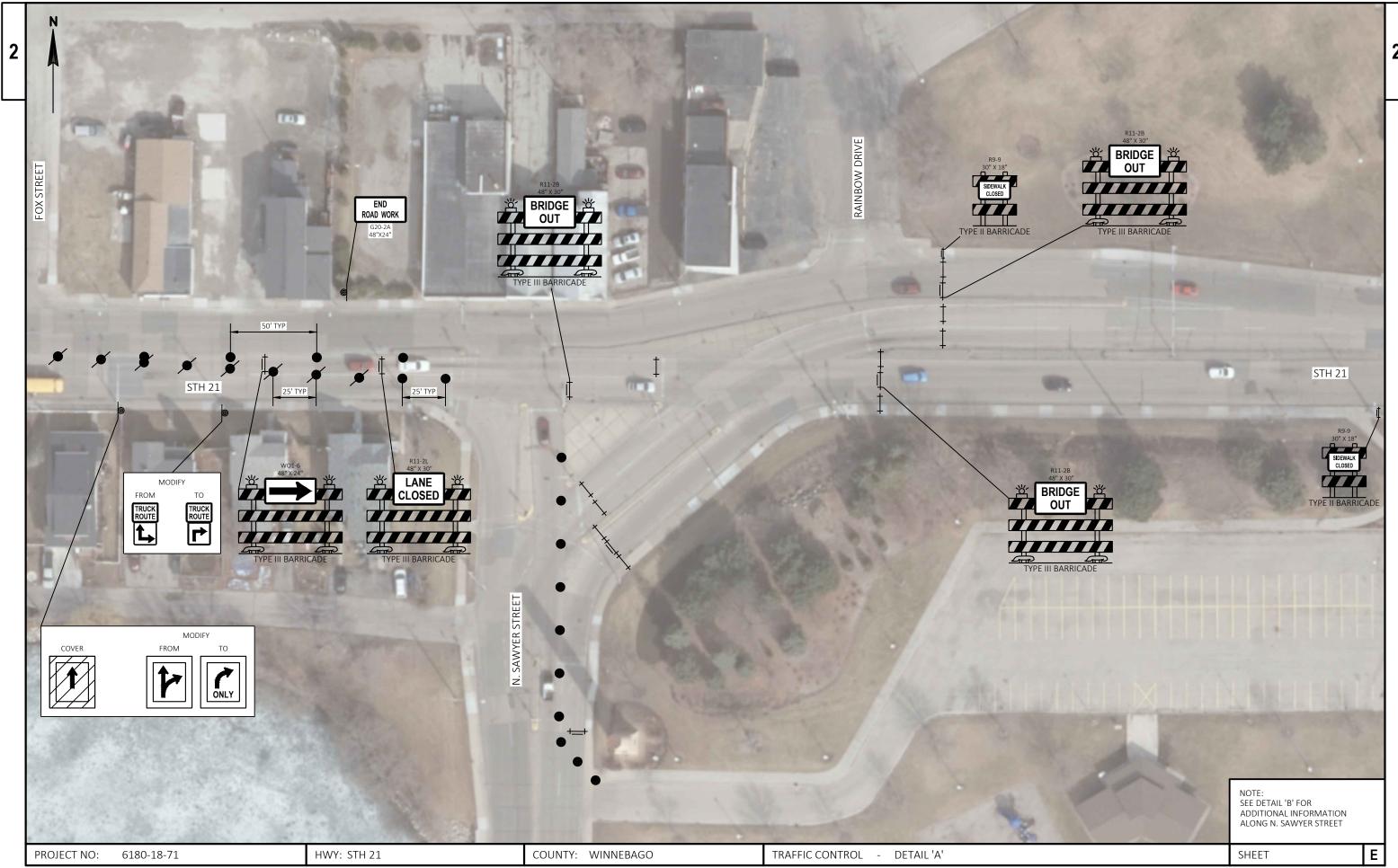






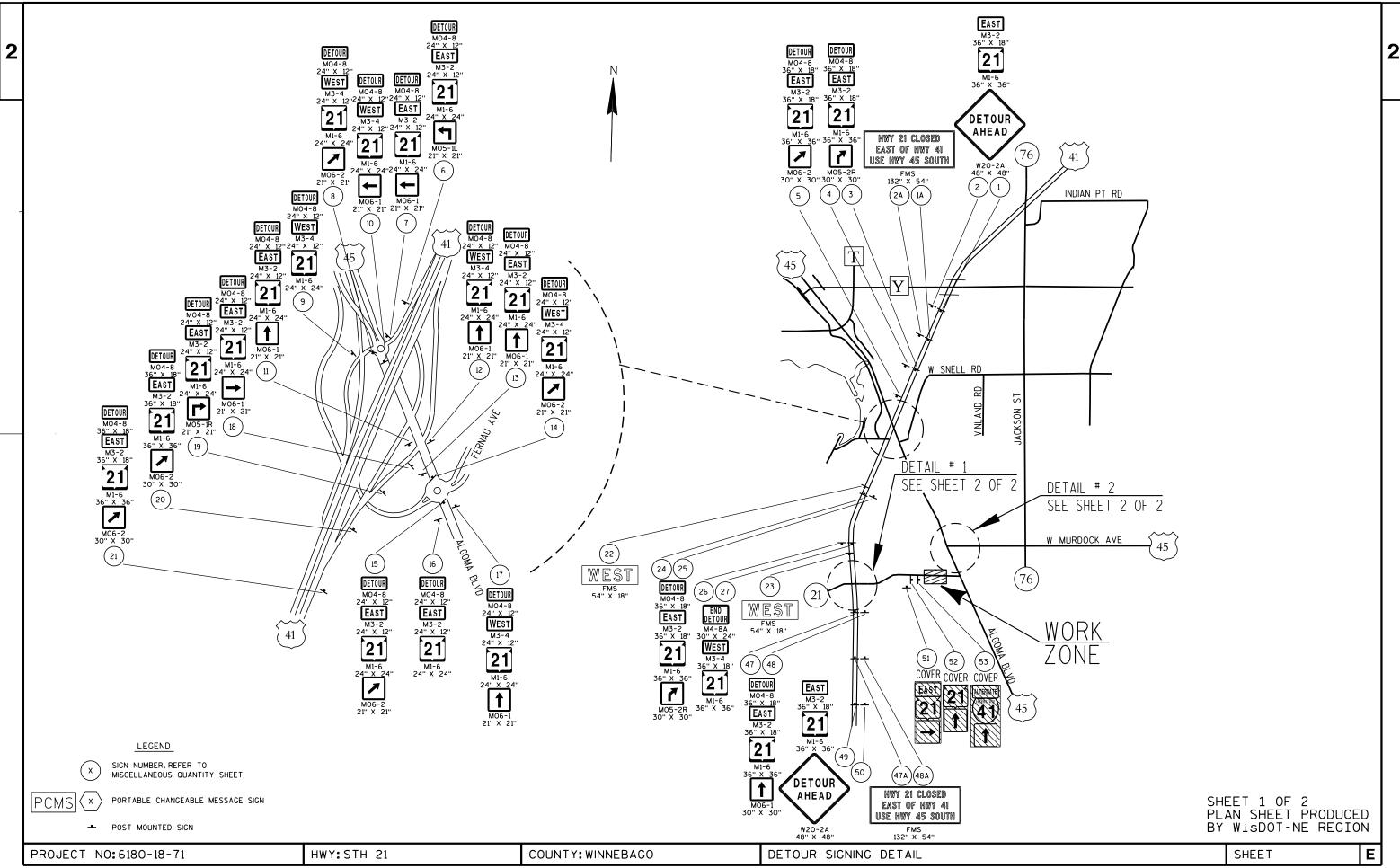


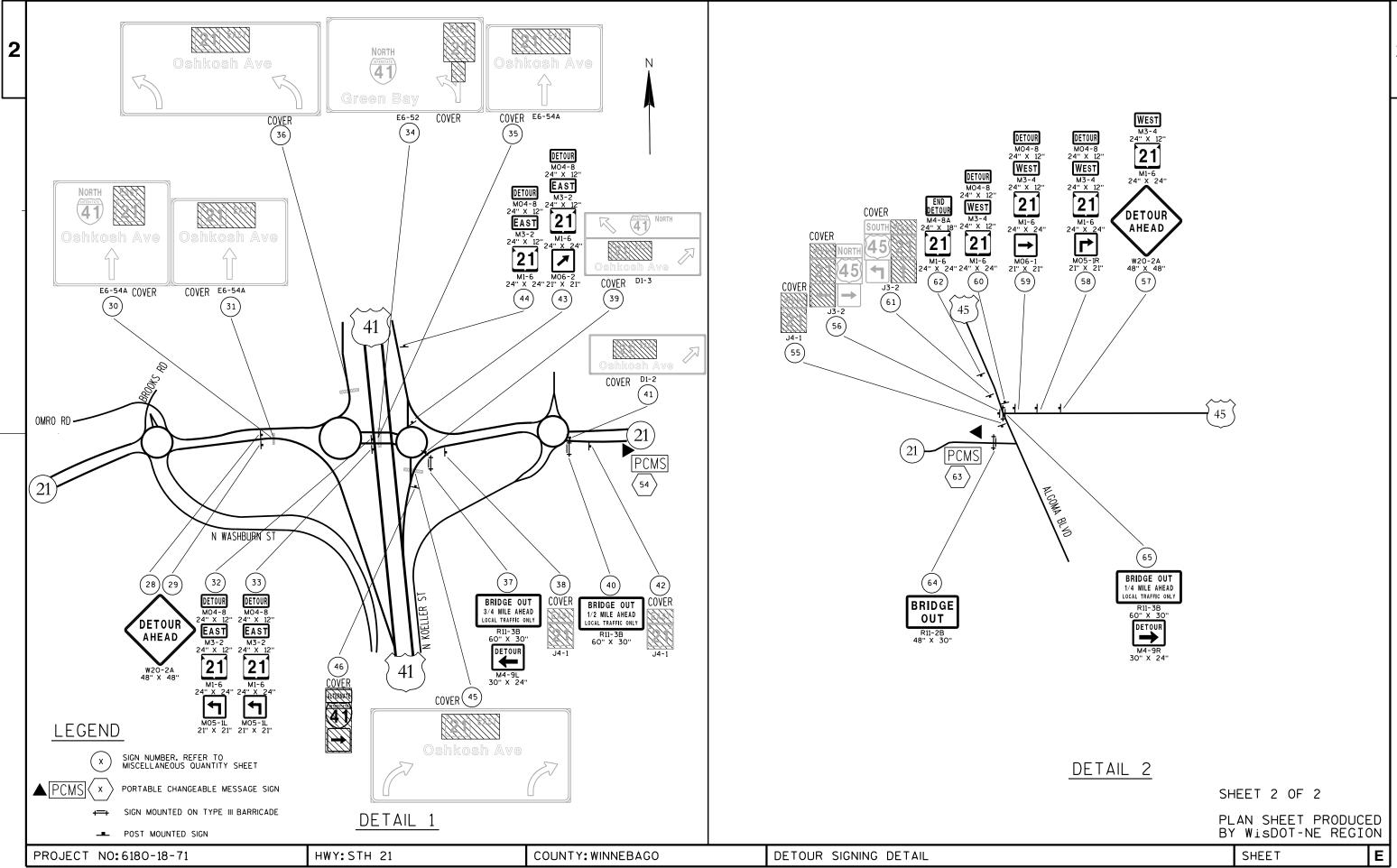












PLOT DATE: 27-MAR-2019 16:13 PLOT BY: dotj1f

PLOT NAME :

PLOT SCALE : 200:1

WISDOT/CADDS SHEET 42

 40	- 4		

					6180-18-71	
Line	Item	Item Description	Unit	Total	Qty	
0002	204.0100	Removing Pavement	SY	870.000	870.000	
0004	204.0155	Removing Concrete Sidewalk	SY	195.000	195.000	
0006	213.0100	Finishing Roadway (project) 01. 6180-18-71	EACH	1.000	1.000	
8000	305.0110	Base Aggregate Dense 3/4-Inch	TON	35.000	35.000	
0010	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	140.000	140.000	
0012	415.0090	Concrete Pavement 9-Inch	SY	635.000	635.000	
0014	415.0410	Concrete Pavement Approach Slab	SY	155.000	155.000	
0016	416.0620	Drilled Dowel Bars	EACH	64.000	64.000	
0018	509.0301	Preparation Decks Type 1	SY	95.000	95.000	
0020	509.0302	Preparation Decks Type 2	SY	1.000	1.000	
0022	509.0310.S	Sawing Pavement Deck Preparation Areas	LF	320.000	320.000	
0024	509.2000	Full-Depth Deck Repair	SY	1.000	1.000	
0026	509.2100.S	Concrete Masonry Deck Repair	CY	6.000	6.000	
0028	509.5100.S	Polymer Overlay	SY	3,551.000	3,551.000	
0030	514.2625	Downspout 6-Inch	LF	24.000	24.000	
0032	517.1800.S	Structure Repainting Recycled Abrasive (structure) 01. B-70-91	LS	1.000	1.000	
0034	517.3000.S	Structure Overcoating Cleaning and Priming (structure) 01. B-70-91	LS	1.000	1.000	
0036	517.4000.S	Containment and Collection of Waste Materials (structure) 01. B-70-91	LS	1.000	1.000	
0038	517.4500.S	Negative Pressure Containment and Collection of Waste Materials (structure) 01. B-70-91	LS	1.000	1.000	
0040	517.6001.S	Portable Decontamination Facility	EACH	1.000	1.000	
0042	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	292.000	292.000	
0044	602.0415	Concrete Sidewalk 6-Inch	SF	1,700.000	1,700.000	
0046	619.1000	Mobilization	EACH	1.000	1.000	
0048	625.0100	Topsoil	SY	119.000	119.000	
0050	628.1504	Silt Fence	LF	235.000	235.000	
0052	628.1520	Silt Fence Maintenance	LF	235.000	235.000	
0054	628.1905	Mobilizations Erosion Control	EACH	6.000	6.000	
0056	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000	
0058	628.2006	Erosion Mat Urban Class I Type A	SY	119.000	119.000	
0060	628.7015	Inlet Protection Type C	EACH	4.000	4.000	
0062	629.0210	Fertilizer Type B	CWT	0.080	0.080	
0064	630.0140	Seeding Mixture No. 40	LB	2.300	2.300	
0066	642.5001	Field Office Type B	EACH	1.000	1.000	
0068	643.0300	Traffic Control Drums	DAY	5,600.000	5,600.000	
0070	643.0410	Traffic Control Barricades Type II	DAY	640.000	640.000	
0072	643.0420	Traffic Control Barricades Type III	DAY	5,108.000	5,108.000	
0074	643.0705	Traffic Control Warning Lights Type A	DAY	9,256.000	9,256.000	

Page 2

Estimate Of Quantitie	S
6180-18-71	

Line	Item	Item Description	Unit	Total	Qty
0076	643.0715	Traffic Control Warning Lights Type C	DAY	2,240.000	2,240.000
0078	643.0900	Traffic Control Signs	DAY	27,562.000	27,562.000
0800	643.0910	Traffic Control Covering Signs Type I	EACH	6.000	6.000
0082	643.0920	Traffic Control Covering Signs Type II	EACH	11.000	11.000
0084	643.1000	Traffic Control Signs Fixed Message	SF	211.500	211.500
0086	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
8800	643.5000	Traffic Control	EACH	1.000	1.000
0090	646.1020	Marking Line Epoxy 4-Inch	LF	1,587.000	1,587.000
0092	646.6120	Marking Stop Line Epoxy 18-Inch	LF	52.000	52.000
0094	650.4500	Construction Staking Subgrade	LF	146.000	146.000
0096	650.7000	Construction Staking Concrete Pavement	LF	146.000	146.000
0098	650.9910	Construction Staking Supplemental Control (project) 01. 6180-18-71	LS	1.000	1.000
0100	690.0250	Sawing Concrete	LF	131.000	131.000
0102	715.0415	Incentive Strength Concrete Pavement	DOL	500.000	500.000
0104	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	1,200.000	1,200.000
0106	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000
0108	SPV.0105	Special 01. Balance Bascule Bridge Leaves	LS	1.000	1.000

REMOVING ITEMS					В	ASE AGGREGAT	E DENSE ITEMS				CONCRETI	E PAVEMENT ITEM	<u>s</u>	
204.010 REMOVI PAVEME STATION - STATION LOCATION SY CATEGORY CODE 0010	ING REMOV ENT CONCR SIDEWA	ING ETE ALK			STATION - STATION	E Location	305.0110	305.0120 BASE AGGREGATE DENSE 11/4-INCH TON		STATION	I - STATION I	415.009 CONCRE PAVEME 9-INCH	TE CONCRETE ON PAVEMENT	
32+50 - 32+65 LT & RT 188	31				TEGORY CODE 0010					CATEGORY CO		LOCATION 31	31	
38+85 - 39+40 LT & RT 327	74				32+50 - 32+81 38+85 - 39+40	LT & RT LT & RT	6 14	30 55				LT & RT 87 LT & RT 228	86 69	
CATEGORY CODE 0010 SUBTOTALS 515 CATEGORY CODE 0020	105				CATEGORY CODE 0		20	85			GORY CODE 0010 S	SUBTOTALS 315		
39+40 - 40+00 LT & RT 355	90			<u>CA</u>	TEGORY CODE 0020 39+40 - 40+00	0 LT & RT	15			CATEGORYCO				
CATEGORY CODE 0020 SUBTOTALS 355	90				CATEGORY CODE 0		15	55			0 - 40+00 GORY CODE 0020 S	LT & RT 320 SUBTOTALS 320		
TOTALS 870	195					TOTALS	35	140				TOTALS 635	155	
					CONCRET	TE CURB AND G	UTTER 30-INCH	TYPE A			CONCRETE	SIDEWALK 6-INCI	<u> </u>	
DRILLED DOWEL BARS					STATION -		LOCATION	601.0409 LF			ION - STATION	LOCATION	602.0415 SF	
<u>DRILLED DOWEL BARS</u> STATION - STATION LOCATION	416.0620 EACH				CATEGORY CODE (32+81	LT & RT	61			+50 - 32+81	LT & RT	270	
CATEGORY CODE 0010	LACIT				32+85 -	CATEGORY CODE 00	LT & RT	111		38	+85 - 39+40	LT & RT CODE 0010 SUBTOTA	650 L 920	
32+50 LT & RT 40+00 LT & RT	32 32				CATEGORY CODE (CATEGORY CO		CODE OUT O SOUTOTA		
TOTAL	64				39+40 -	40+00	LT & RT	120		39	+40 -40+00	LT & RT	780	
					C	CATEGORY CODE 00	020 SUBTOTALS	120			CATEGORY	CODE 0020 SUBTOTA	L 780	
							TOTALS	292				тота	L 1,700	
	RESTORATIO	N ITEMS												
	625.0100 TOPSOIL	628.2006 EROSION MAT URBAN CLASS I	629.0210 FERTILIZER TYPE B	630.0140 SEEDING MIXTURE					<u>ER0</u> 628.1504	OSION CONTRO 628.1520	OL ITEMS 628.1905	628.1910	628.7015	
STATION - STATION LOCATION CATEGORY CODE 0010	SY	TYPE A SY	CWT	NO. 40	_				SILT FENCE		MOBILIZATIONS EROSION		INLET	
39+04 - 39+40 LT	20	20	0.01	0.4	_			STATION	LF	LF	EACH	EACH	EACH	
39+24 - 39+40 RT UNDISTRIBUTED	9 7	9 7	0.01 0.01	0.2 0.2				CATEGORY CODE 0010 PROJECT 6180-18-71			6	3	4	
CATEGORY CODE 0010 SUBTOTALS	36	36	0.03	0.8	<u> </u>			38+85 - 39+40	115	115				
CATEGORY CODE 0020					_			CATEGORY CODE 0010 SU	BTOTALS 115	115	6	3	4	
39+40 - 40+00 LT & RT	66	66	0.04	1.2				CATEGORY CODE 0020						

COUNTY: WINNEBAGO MISCELLANEOUS QUANTITIES PROJECT NO: 6180-18-71 HWY: STH 21 SHEET S:\CURRPROJ\WINNEBCO\STH 21 - FOX RIVER BRIDGE\CIVIL3D\61801800\SHEETSPLAN\61801871-030101-MQ.DWG LAYOUT NAME - Sheet - 01 FILE NAME : PLOT BY: AARON SARAUER PLOT NAME : PLOT SCALE :

CATEGORY CODE 0020 SUBTOTALS

TOTALS

119

NOTES: NO FERTILIZER SHALL BE PLACED WITHIN 20 FEET OF THE FOX RIVER.

83

119

0.05

0.08

1.5

2.3

TOTALS 235

CATEGORY CODE 0020 SUBTOTALS

120

235

3

E

646.6120 MARKING STOP LINE **EPOXY** 18-INCH WHITE

> 26 26

52

646.1020

PAVEMENT MARKING ITEMS

	NUMBER OF	643.0300 643.0410 TRAFFIC TRAFFIC CONTROL CONTROL DRUMS BARRICADES		643.0420* 643.070 TRAFFIC TRAFFI CONTROL CONTROL BARRICADES WARNIN TYPE III LIGHTS TY		AFFIC NTROL RNING	TRAFFIC CONTROL WARNING		TRAFFIC CONTROL SIGNS		643.0920* TRAFFIC CONTROL COVERING SIGNS TYPE II		L G	STATIC CATEGORY CO	DN - STATION	LOCATION	MAI L EP	RKING INE POXY NCH YELLOW LF			
	DAYS IN	NO.		NO.	TOTAL	NO.	TOTAL	NO.	TOTAL	NO.	TOTAL	NO.	TOTAL	NO.	NO.	TOTAL		50 - 35+17	LT & RT	150	534
CATEGORY CODE 0010	SERVICE	REQ'D	DAY	REQ'D	DAY	REQ'D	DAY	REQ'D	DAY	REQ'D	DAY	REQ'D	DAY	CYCLES	SIGNS	EACH		19 - 39+40	LT & RT	150	583
STH 21 WEST OF PROJECT N. SAWYER STREET WEST OF PROJECT		14 21	2,240 3,360	2	320 	11 8	1,760 1,280	20 14	3,200 2,240	8 6	1,280 960	15 8	2,400 1,280	1	1	1	CATEGORY CC		DE 0010 SUBTOTALS	300	1,117
STH 21 EAST OF PROJECT	160			2	320	9	1,440	16	2,560			6	960				39+	10 - 40+00	LT & RT	50	120
	TOTALS		5,600		640		4,480		8,000		2,240		4,640			1		CATEGORY CO	DE 0020 SUBTOTALS	50	120
* ADDITIONAL QUANTITIES SHOWN EL	SEWHERE IN P	LANS															-			350	1,237
																			TOTAL	1,	587

CONSTRUCTION STAKING ITEMS

		650.4500 SUBGRADE	650.7000 CONCRETE PAVEMENT	650.9910 SUPPLEMENTAL CONTROL
STATION - STATION	LOCATION	LF	LF	LS
CATEGORY CODE 0010				
PROJECT 6180-18-71				1
32+50 - 32+81	LT & RT	31	31	-
38+85 - 39+40	LT & RT	55	55	
CATEGORY CODE 0020)10 SUBTOTALS	86	86	1
39+40 - 40+00	LT & RT	60	60	
CATEGORY CODE 00	20 SUBTOTALS	60	60	
	TOTALS	146	146	1

SAWING CONCRETE

		690.0250
STATION	LOCATION	LF
CATEGORY CODE C	0010	
32+50	LT & RT	65
40+00	LT & RT	66
	TOTAL	131
	IOIAL	131

E COUNTY: WINNEBAGO SHEET PROJECT NO: 6180-18-71 HWY: STH 21 MISCELLANEOUS QUANTITIES FILE NAME :

TRAFFIC CONTROL DETOUR SIGN SUMMARY

	SI GN		SI GN	SIZE	NUMBER I N	APPROX. SERVI CE PERI OD 157	643. 0900 SI GNS	643. 0420 BARRI CADES TYPE III	643. 0705 WARNI NG LI GHTS TYPE A	643. 1000 SI GNS FI XED MESSAGE	643. 1050 SI GNS PORTABLE CHANGEABLE MESSAGE	NO OF CYCLES	643. 0910 COVERI NG SI GNS TYPE I	643. 0920 COVERI NG SI GNS TYPE 11	
	NO.	LOCATI ON	CODE	WXH	SERVI CE		DAYS	DAYS	DAYS	SF	DAYS	CICLES	EACH	EACH	REMARKS
	1	I-41, N. OF USH 45, PLACE 1/2 MILE N. OF USH 45 OFF-RAMP IN MEDIAN	М 3-2	36"X18"	1	157	157								
-		II WEDIAN	M 1-6	36"X36"	1	157	157								21
		п	W 20-2A	48"X48"	1	157	157								
	1A	I-41, N. OF USH 45, PLACE 2000' N. OF USH 45 OFF-RAMP IN MEDIAN	FMS	132"X54"						49. 5					SEE SIGN DETAIL SHE
	2	I-41, N. OF USH 45, PLACE 1/2 MILE N. OF USH 45 OFF-RAMP ON RIGHT SHOULDER	М 3-2	36"X18"	1	157	157								
		n .	M 1-6	36"X36"	1	157	157								21
		I - 41, N. OF USH 45, PLACE 2000' N. OF USH 45 OFF-RAMP ON	W 20-2A	48"X48"	1	157	157								
-	2A	RI GHT SHOULDER	FMS	132"X54"						49. 5					SEE SIGN DETAIL SHE
-	3	I - 41, N. OF USH 45, PLACE 1/4 MILE N. OF USH 45 OFF-RAMP IN MEDIAN	MO 4-8	36"X18"	1	157	157								
-		" "	M 3-2	36"X18"	1	157	157				1				0.1
}		n	M 1-6 MO 5-2R	36"X36" 30"X30"	1	157 157	157 157			-	+		1		21
}		I-41, N. OF USH 45, PLACE 1/4 MILE N. OF USH 45 OFF-RAMP													
-	4	ON RIGHT SHOULDER	MO 4-8 M 3-2	36"X18" 36"X18"	1	157 157	157 157								
		"	M 1-6	36"X36"	1	157	157								21
ľ		п	MO 5-2R	30"X30"	1	157	157								
	5	I-41, N. OF USH 45, PLACE 250' N. OF USH 45 OFF-RAMP ON RIGHT SHOULDER	MO 4-8	36"X18"	1	157	157								
		II .	М 3-2	36"X18"	1	157	157								
		n .	M 1-6	36"X36"	1	157	157								21
-		I A1 OFF DAME TO HELL AT DIAGE TOO! DELOE TO HELL AT DAME	MO 6-2	30"X30"	1	157	157								TILT RIGHT
-	6	I - 41 OFF-RAMP TO USH 45, PLACE 500' PRIOR TO USH 45 RAMP TERMINI	MO 4-8	24"X12"	1	157	157								
-		" "	M 3-2	24"X12"	1	157	157				1				0.1
		п	M 1-6 MO 5-1L	24"X24" 21"X21"	1 1	157 157	157 157				1				21
-	7	I-41 OFF-RAMP TO USH 45, PLACE 100' PRIOR TO USH 45 RAMP TERMINI	MO 4-8	24"X12"	1	157	157								
ľ		II	М 3-2	24"X12"	1	157	157								
		"	М 1-6	24"X24"	1	157	157								21
		"	MO 6-1	21"X21"	1	157	157								LEFT
_	8	USH 45, PLACE ABOVE SPLITTER ISLAND SIGN AT RAB RAMP TERMINI	MO 4-8	24"X12"	1	157	157								
		"	M 3-4	24"X12"	1	157	157								0.1
}		п	M 1-6 MO 6-2	24"X24" 21"X21"	1 1	157 157	157 157								21 TILT RIGHT
ŀ	9	USH 45 ON-RAMP TO I-41 SB, PLACE 400' S. OF RAB RAMP TERMINI	MO 4-8	24"X12"	1	157	157								ILI RIGHT
ŀ		I ERMI NI	М 3-4	24"X12"	1	157	157				+ +		1		
ļ		п	M 1-6	24"X24"	1	157	157								21
-	10	USH 45, S. OF I-41 SB RAMP TERMINI, PLACE 100' S. OF RAB RAMP TERMINI	MO 4-8	24"X12"	1	157	157								
		"	М 3-4	24"X12"	1	157	157								
		n .	M 1-6	24"X24"	1	157	157				<u> </u>				21
}		USH 45, N. OF I-41 NB RAMP TERMINI, PLACE 100' N. OF RAB	MO 6-1	21"X21"	1	157	157						-		LEFT
	11	USH 45, N. OF 1-41 NB RAMP TERMINI, PLACE 100° N. OF RAB RAMP TERMINI	MO 4-8	24"X12"	1	157	157								
}		" "	M 3-2 M 1-6	24"X12" 24"X24"	1 1	157 157	157 157								21
}		"	MO 6-1	24 X24 21"X21"	1	157	157								AHEAD
-	12	USH 45, S. OF I-41 NB RAMP TERMINI, PLACE 200' S. OF RAB RAMP TERMINI	MO 4-8	24"X12"	1	157	157								MILIA
ŀ		AVELUM A LAWVIE ITE	M 3-4	24"X12"	1						† †				
		"	М 1-6	24"X24"	1	157	157								21
] I SHEET F		JCED PAGE SUBTOTALS	MO 6-1	21"X21"	1 44	157	157 6, 908	0	0	99. 0	0		0	0	AHEAD

BY WisDOT - NE REGION

TRAFFIC CONTROL DETOUR SIGN SUMMARY

					ADDDOV	643. 0900	643. 0420	643. 0705	643. 1000	643. 1050		643. 0910	643. 0920	
				NUMBER	APPROX. SERVI CE	SIGNS	BARRI CADES TYPE III	WARNI NG LI GHTS	SI GNS FI XED	SI GNS PORTABLE		COVERI NG SI GNS	COVERI NG SI GNS	
SIGN		SIGN	SI ZE	IN	PERI OD		TIPE III	TYPE A	MESSAGE	CHANGEABLE	NO OF	TYPE I	TYPE II	
Siun		SIGN	SIZE	IN	157			IIILA	WESSAGE	MESSAGE	CYCLES	111111	11112 11	
NO.	LOCATI ON	CODE	WХН	SERVI CE	DAYS	DAYS	DAYS	DAYS	SF	DAYS	CICLES	EACH	EACH	REMARKS
13	USH 45, N. OF FERNAU AVE, PLACE 200' N. OF FERNAU AVE RAB	MO 4-8	24"X12"	1	157	157	Dillo	Dillo	SI .	Dillo		I I I I I I I I I I I I I I I I I I I	Liton	ICLIVE HOLES
10	"	M 3-2	24"X12"	1	157	157								
	· ·	M 1-6	24"X24"	1	157	157								21
	0	MO 6-1	21"X21"	1	157	157								AHEAD
14	USH 45, AT FERNAU AVE, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT EXIT TO USH 45 NB	MO 4-8	24"X12"	1	157	157								
	H .	М 3-4	24"X12"	1	157	157								
	H .	М 1-6	24"X24"	1	157	157								21
	11	MO 6-2	21"X21"	1	157	157								TILT RIGHT
15	USH 45, AT FERNAU AVE, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN AT EXIT TO USH 45 SB	MO 4-8	24"X12"	1	157	157								
	II .	М 3-2	24"X12"	1	157	157								
	II .	М 1-6	24"X24"	1	157	157								21
	ti .	MO 6-2	21"X21"	1	157	157								TILT RIGHT
16	USH 45, S. OF FERNAU AVE, PLACE RIGHT OF EXISTING J4-1 SIGN	MO 4-8	24"X12"	1	157	157								
	u .	М 3-2	24"X12"	1	157	157								
	ti .	М 1-6	24"X24"	1	157	157								21
17	USH 45, S. OF FERNAU AVE, PLACE 200' S. OF FERNAU AVE RAB	MO 4-8	24"X12"	1	157	157								
	II.	М 3-4	24"X12"	1	157	157								
	"	М 1-6	24"X24"	1	157	157								21
	II .	MO 6-1	21"X21"	1	157	157								AHEAD
18	I-41 NB TO USH 45 SB RAMP, PLACE RIGHT OF EXISTING R1-1 SIGN	MO 4-8	24"X12"	1	157	157								
	II .	М 3-2	24"X12"	1	157	157								
	"	М 1-6	24"X24"	1	157	157								21
	"	MO 6-1	21"X21"	1	157	157								RI GHT
19	I-41 NB TO USH 45 SB RAMP, PLACE RIGHT OF EXISTING R1-1 SIGN	MO 4-8	24"X12"	1	157	157								
	"	М 3-2	24"X12"	1	157	157								
	"	M 1-6	24"X24"	1	157	157								21
	" I A4 ND TO UCH AT DAME DIAGE IN EDONE OF C 70 00 CIGN	MO 5-1R	21"X21"	1	157	157								
20	I-41 NB TO USH 45 RAMP, PLACE IN FRONT OF S-70-66 SIGN BRIDGE	MO 4-8	36"X18"	1	157	157								
	"	M 3-2	36"X18"	1	157	157			ļ					
	"	M 1-6	36"X36"	1	157	157								21
		MO 6-2	30"X30"	1	157	157								TILT RIGHT
21	I-41, AT USH 45 EXIT RAMP, PLACE IN FRONT OF S-70-65 SIGN BRIDGE	MO 4-8	36"X18"	1	157	157								
	"	M 3-2	36"X18"	1	157	157								
	"	M 1-6	36"X36"	1	157	157								21
	"	MO 6-2	30"X30"	1	157	157								TILT RIGHT
22	I-41, S. OF USH 45, PLACE ON SIGN BRIDGE TYPE I SIGN NEXT TO 21 ROUTE MARKER	FMS	54"X18"	1					6. 75					WEST
23	I-41, S. OF USH 45, PLACE ON SIGN BRIDGE TYPE I SIGN NEXT TO 21 ROUTE MARKER	FMS	54"X18"	1					6. 75					WEST
	PAGE SUBTOTALS			37	·	5, 495	0	0	13. 50	0		0	0	

PLAN SHEET PRODUCED

BY WisDOT - NE REGION

TRAFFIC CONTROL DETOUR SIGN SUMMARY

S	SI GN		SI GN	SIZE	NUMBER I N	APPROX. SERVI CE PERI OD 157	643. 0900 SI GNS	643. 0420 BARRI CADES TYPE 111	643. 0705 WARNI NG LI GHTS TYPE A	643. 1000 SI GNS FI XED MESSAGE	643. 1050 SI GNS PORTABLE CHANGEABLE MESSAGE	NO OF CYCLES	643. 0910 COVERI NG SI GNS TYPE I	643. 0920 COVERING SIGNS TYPE II	
ľ	NO.	LOCATI ON	CODE	WXH	SERVI CE	DAYS	DAYS	DAYS	DAYS	SF	DAYS		EACH	EACH	REMARKS
	24	I-41, S. OF USH 45, PLACE 1700' S. OF USH 45 EXIT RAMP IN MEDIAN	MO 4-8	36"X18"	1	157	157								
		II .	М 3-2	36"X18"	1	157	157								
		"	M 1-6	36"X36"	1	157	157								
	25	I - 41, S. OF USH 45, PLACE 1700' S. OF USH 45 EXIT RAMP ON RIGHT SHOULDER	MO 5-2R MO 4-8	30"X30" 36"X18"	1	157 157	157 157								
		II II	М 3-2	36"X18"	1	157	157								
		ıı .	M 1-6	36"X36"	1	157	157								
		11	MO 5-2R	30"X30"	1	157	157								
	26	I-41, S. OF USH 45, PLACE IN FRONT OF S-70-62 SIGN BRIDGE ON RIGHT SHOULDER	M 4-8A	30"X24"	1	157	157								
		"	М 3-4	36"X18"	1	157	157								
		"	M 1-6	36"X36"	1	157	157								
	27	I-41, S. OF USH 45, PLACE IN FRONT OF S-70-62 SIGN BRIDGE ON RIGHT SHOULDER	M 4-8A	30"X24"	1	157	157								
		"	M 3-4	36"X18"	1	157	157								
	28	STH 21, W. OF I-41, PLACE 500' W. OF I-41 SB RAMP TERMINI IN MEDIAN	M 1-6 W 20-2A	36"X36" 48"X48"	1	157 157	157 157								
	29	STH 21, W. OF I-41, PLACE 500' W. OF I-41 SB RAMP TERMINI ON RIGHT SHOULDER	W 20-2A	48"X48"	1	157	157								
	30	STH 21, W. OF I-41, COVER SIGNS ON S-70-102 SIGN BRIDGE AS SHOWN										1	1		COVER "21 EAST"
	31	STH 21, W. OF I-41, COVER SIGNS ON S-70-102 SIGN BRIDGE AS SHOWN										1	1		COVER "21 EAST"
	32	STH 21, AT I-41, PLACE AT S-70-104 IN MEDIAN	MO 4-8	24"X12"	1	157	157								
		"	М 3-2	24"X12"	1	157	157								
		"	M 1-6	24"X24"	1	157	157								21
	00	CONTROL AT I 44 DIAGE AT C 70 104 ON DIGHT CIDE	MO 5-1L	21"X21"	1	157	157								
	33	STH 21, AT I-41, PLACE AT S-70-104 ON RIGHT SIDE	MO 4-8 M 3-2	24"X12" 24"X12"	1 1	157 157	157 157								
		11	M 1-6	24"X24"	1 1	157	157								21
		"	MO 5-1L	21"X21"	1	157	157								W.1
	34	STH 21, AT I-41, COVER SIGNS ON S-70-104 SIGN BRIDGE AS SHOWN										1	1		COVER "21 EAST"
	35	STH 21, AT I-41, COVER SIGNS ON S-70-104 SIGN BRIDGE AS SHOWN										1	1		COVER "21 EAST"
	36	I-41 OFF-RAMP TO STH 21, COVER EXISTING TYPE I SIGN AS SHOWN										1	1		COVER "21 EAST"
	37	STH 21, AT I-41 NB RAMP TERMINI, PLACE ON RIGHT SHOULDER	R 11-3B	60"X30"	1	157	157	157	314						3/4 MI LE AHEAD
_	00	CONTROL DE LA COMPTE PAR CONTROL TA O CALON AS CALON AS	M 4-9L	30"X24"	1	157	157								COLUED ENTER DE CLOS
\vdash	38	STH 21, E. OF I-41, COVER EXISTING J4-2 SIGN AS SHOWN STH 21, AT I-41 NB RAMP TERMINI, COVER EXISTING D1-3 SIGN										1		1	COVER ENTIRE SIGN
	39	AS SHOWN	D 44	00077777		4		4.5.5	97.7			1		1	COVER "21 EAST"
<u> </u>	40	STH 21, AT KOELLER ST RAB, PLACE ON RIGHT SHOULDER	R 11-3B	60"X30"	1	157	157	157	314				-		
	41	STH 21, AT KOELLER ST RAB, COVER EXISTING D1-2 SIGN AS SHOWN										1		1	COVER "21 EAST"
	42	STH 21, E. OF KOELLER ST, COVER EXISTING J4-2 SIGN AS SHOWN PAGE SUBTOTALS			27		4, 239	314	628	0	0	1	5	1 4	

BY WisDOT - NE REGION

TRAFFIC CONTROL DETOUR SIGN SUMMARY

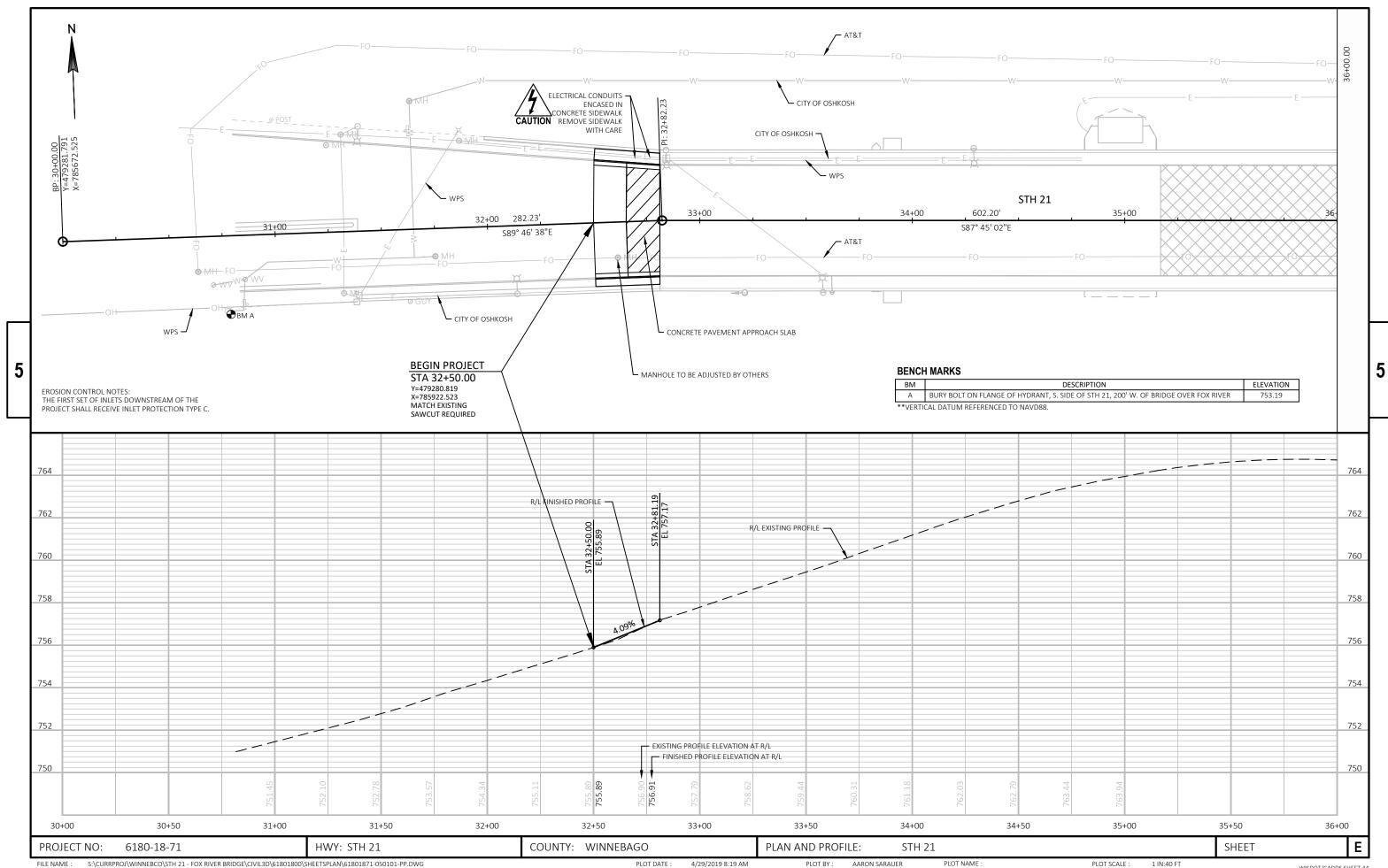
SI GN		SI GN	SI ZE	NUMBER I N	APPROX. SERVI CE PERI OD 157	643. 0900 SI GNS	643. 0420 BARRI CADES TYPE III	LI GHTS TYPE A	643. 1000 SI GNS FI XED MESSAGE	643. 1050 SI GNS PORTABLE CHANGEABLE MESSAGE	NO OF CYCLES	643. 0910 COVERI NG SI GNS TYPE I	643. 0920 COVERI NG SI GNS TYPE II	
NO.	LOCATI ON STH 21, AT I-41 NB RAB, PLACE ABOVE EXISTING SPLITTER	CODE	WXH	SERVI CE		DAYS	DAYS	DAYS	SF	DAYS		EACH	EACH	REMARKS
43	ISLAND	MO 4-8	24"X12"	1	157	157								
	II.	М 3-2	24"X12"	1	157	157								
	II	М 1-6	24"X24"	1	157	157								21
	II .	MO 6-2	21"X21"	1	157	157								TILT RIGHT
44	STH 21 ON-RAMP TO I-41, PLACE 750' N. OF STH 21 RAB	MO 4-8	24"X12"	1	157	157								
	"	M 3-2	24"X12"	1	157	157								0.1
45	I-41 OFF-RAMP TO STH 21, COVER SIGN ON S-70-106 SIGN BRIDGE AS SHOWN	M 1-6	24"X24"	1	157	157					1	1		21
46	I-41 OFF-RAMP TO STH 21, COVER EXISTING J3-1 SIGN AS SHOWN										1		1	COVER ENTIRE SIGN
47	I-41, S. OF STH 21, PLACE 1/4 MILE S. OF STH 21 EXIT RAMP IN MEDIAN	MO 4-8	36"X18"	1	157	157							-	COVEN ENTINE STUN
	"	М 3-2	36"X18"	1	157	157								
	II .	М 1-6	36"X36"	1	157	157								21
	11	MO 6-1	30"X30"	1	157	157								AHEAD
47A	I-41, S. OF STH 21, PLACE 2000' S. OF STH 21 EXIT RAMP IN MEDIAN	FMS	132"X54"						49. 5					SEE SIGN DETAIL SHEET
48	I-41, S. OF STH 21, PLACE 1/4 MILE S. OF STH 21 EXIT RAMP ON RIGHT SHOULDER	MO 4-8	36"X18"	1	157	157								
	11	М 3-2	36"X18"	1	157	157								
	"	M 1-6	36"X36"	1	157	157								21
	I-41, S. OF STH 21, PLACE 2000' S. OF STH 21 EXIT RAMP ON	MO 6-1	30"X30"	1	157	157								AHEAD
48A	RI GHT SHOULDER	FMS	132"X54"						49. 5					SEE SIGN DETAIL SHEET
49	I-41, S. OF STH 21, PLACE 1/2 MILE S. OF STH 21 EXIT RAMP IN MEDIAN	М 3-2	36"X18"	1	157	157								
	11	М 1-6	36"X36"	1	157	157								21
	"	W 20-2A	48"X48"	1	157	157								
50	I-41, S. OF STH 21, PLACE 1/2 MILE S. OF STH 21 EXIT RAMP ON RIGHT SHOULDER	М 3-2	36"X18"	1	157	157								
	"	M 1-6	36"X36"	1	157	157								21
~ 1	CANAZID OTE ATE OTEN OF CONTROL TO 1 CLON AC CHOIN	W 20-2A	48"X48"	1	157	157								COVER EMELDE CLON
	SAWYER ST, AT STH 21, COVER EXISTING J3-1 SIGN AS SHOWN STH 21, AT SAWYER ST, COVER EXISTING J13-1 SIGN AS SHOWN										<u>1</u> 1		1	COVER ENTIRE SIGN COVER ENTIRE SIGN
	STH 21, W. OF FOX RIVER, COVER EXISTING J3-1 SIGN AS SHOWN										1		1	COVER ENTIRE SIGN
54	STH 21, W. OF FOX REVER, COVER EXISTENCE 33-1 SEGRED AS SHOWN STH 21, E. OF EAGLE ST, PLACE ON RIGHT SHOULDER, FIELD DETERMINED	PCMS		1						7	1		1	PLACE PRIOR TO CLOSURE
55	STH 21, S. OF USH 45, COVER EXISTING J4-1 SIGN AS SHOWN										1		1	COVER ENTIRE SIGN
	STH 21, AT USH 45 INTERSECTION, COVER EXISITNG J3-2 SIGN AS SHOWN										1		1	COVER "WEST 21 LEFT"
57	USH 45, E. OF ALGOMA BLVD, PLACE 1/4 MILE E. OF ALGOMA BLVD INTERSECTION	М 3-4	24"X12"	1	157	157								
	11	M 1-6	24"X24"	1	157	157								21
	HOW AS TO DE ALCOMA DATE BY A CONTROL OF A C	W 20-2A	48"X48"	1	157	157	<u> </u>			 				
58	USH 45, E. OF ALGOMA BLVD, PLACE 750' E. OF ALGOMA BLVD INTERSECTION	MO 4-8	24"X12"	1	157	157								
	"	M 3-4	24"X12"	1	157	157	1							64
	" "	M 1-6	24"X24" 21"X21"	1 1	157 157	157 157				 				21
59	USH 45, E. OF ALGOMA BLVD, PLACE 200' E. OF ALGOMA BLVD INTERSECTION	MO 5-1R MO 4-8	24"X12"	1	157	157								
	I N I EAGECTI UN	М 3-4	24"X12"	1	157	157				+				
	II .	M 1-6	24"X24"	1	157	157	<u> </u>			 				21
	11	MO 6-1	21"X21"	1	157	157								RI GHT
	PAGE SUBTOTALS	•		33		5, 024	0	0	99	7		1	6	

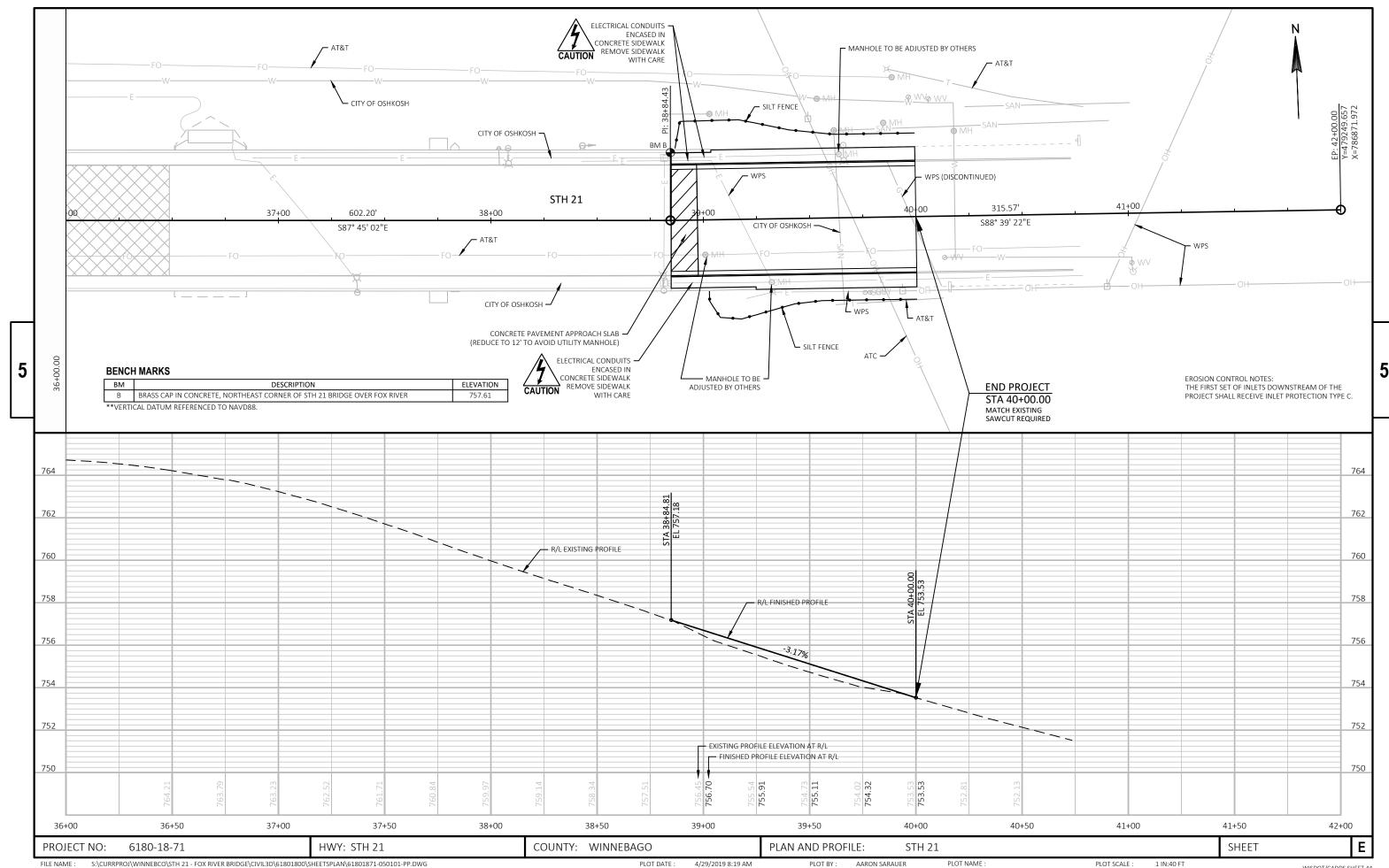
PLAN SHEET PRODUCED BY WisDOT - NE REGION

TRAFFIC CONTROL DETOUR SIGN SUMMARY

						643. 0900	643. 0420	643. 0705	643. 1000			643. 0910	643. 0920	
					APPROX.	SI GNS	BARRI CADES		SI GNS	SI GNS		COVERI NG	COVERI NG	
				NUMBER	SERVI CE		TYPE III	LI GHTS	FI XED	PORTABLE		SI GNS	SI GNS	
SIGN		SI GN	SIZE	IN	PERI OD			TYPE A	MESSAGE	CHANGEABLE	NO OF	TYPE I	TYPE II	
					157					MESSAGE	CYCLES			
NO.	LOCATI ON	CODE	WXH	SERVI CE	DAYS	DAYS	DAYS	DAYS	SF	DAYS		EACH	EACH	REMARKS
60	USH 45, N. OF US 45/STH 21, PLACE 150' N. OF US 45/STH 21 INTERSECTION	MO 4-8	24"X12"	1	157	157								
	n .	М 3-4	24"X12"	1	157	157								
	II .	M 1-6	24"X24"	1	157	157								21
61	USH 45, N. OF US 45/STH 21, COVER EXISTING J3-2 SIGN AS SHOWN													COVER "WEST 21 AHEAD"
62	USH 45, N. OF US 45/STH 21, PLACE 1000' N. OF USH 45/STH 21 INTERSECTION	M 4-8A	24"X18"	1	157	157								
	"	М 1-6	24"X24"	1	157	157								
63	STH 21, E. OF FOX RIVER, PLACE E. OF FOX RIVER BRIDGE, FIELD DETERMINED	PCMS		1						7				PLACE PRIOR TO CLOSURE
64	STH 21, AT CONGRESS AVE/HIGH AVE, PLACE ON RIGHT SHOULDER AT INTERSECTION	R 11-2B	60"X30"	1	157	157	157	314						
65	STH 21, AT USH 45 INTERSECTION, FACING EAST TOWARD USH 45	R 11-3B	60"X30"	1	157	157	157	314						1/4 MI LE AHEAD
	"	M 4-9R	30"X24"	1	157	157								
	PAGE SUBTOTALS			9		1, 256	314	628	0	7		0	0	
	PROJECT TOTALS			150		22, 922	628	1, 256	211.5	14		6	10	

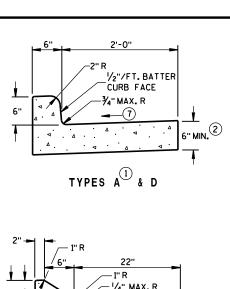
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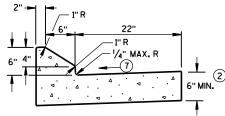




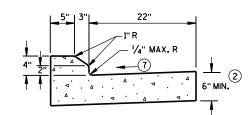
Standard Detail Drawing List

08D01-20A	CONCRETE CURB & GUTTER
08D01-20B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13B02-09A	CONCRETE PAVEMENT APPROACH SLAB
13C01-19	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C13-09	URBAN DOWELED CONCRETE PAVEMENT
13C18-07A	CONCRETE PAVEMENT JOINTING
13С18-07В	CONCRETE PAVEMENT STEEL REINFORCEMENT
13C18-07C	CONCRETE PAVEMENT JOINT TYPES
13C18-07D	CONCRETE PAVEMENT JOINT TYPES AT UTILITY FIXTURES
15C02-07A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-07B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-07C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15С11-07В	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C33-03	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D20-04	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D21-06	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS

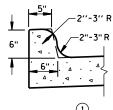




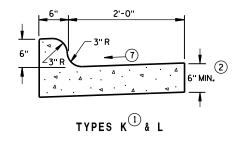
6" SLOPED CURB TYPES G 4 J



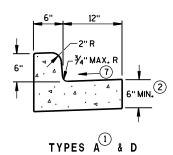
4" SLOPED CURB TYPES G 4 J



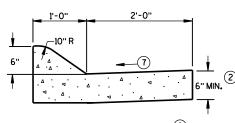
TYPES K (1) & L (OPTIONAL CURB SHAPE)



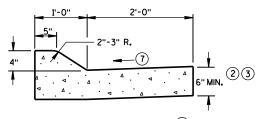
CONCRETE CURB & GUTTER 30"



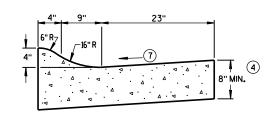
CONCRETE CURB & GUTTER 18"



6" SLOPED CURB TYPES A & D

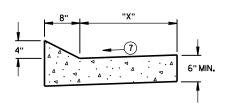


4" SLOPED CURB TYPES A D



4" SLOPED CURB TYPES R T & T

CONCRETE CURB & GUTTER 36"



TYPES TBT & TBTT

CONCRETE CURB & GUTTER

TBT & TBTT	"X"
30"	22"
36"	28"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-O" BEHIND THE BACK OF CURBS.

- (1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.
- 2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- (3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED
- (4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- (5) THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- (6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- (7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- (8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'

6

20a

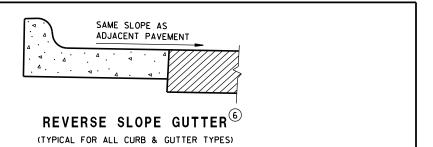
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CONCRETE PANEL WIDTH SAME PAY LIMITS TRAFFIC TRAFFIC LANE -AS CURB & GUTTER LANE PAVEMENT SLOPE PAVEMENT THICKNESS

PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER



CONCRETE CURB & GUTTER

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

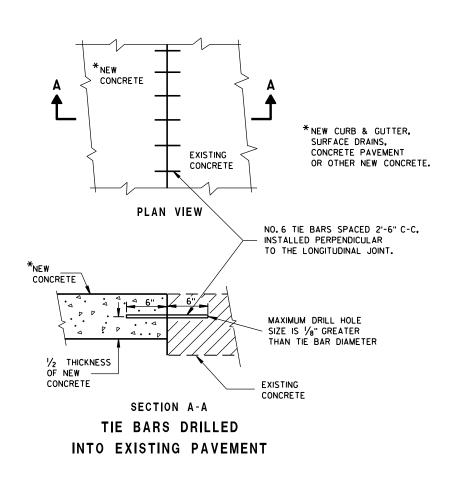
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^{*} BIKE LANE IS NOT SHOWN.

DETAIL OF CURB AND GUTTER AT INLETS (TYPE H INLET COVER SHOWN)

CONTRACTION PAVEMENT

END SECTION CURB & GUTTER



GENERAL NOTES

_ 1/2"/FT.BATTER,FACE OF CURB (ABOVE ADJACENT PAVEMENT)

ADJACENT

PAVEMENT

NO. 4 X 2'-0" DEF. TIE

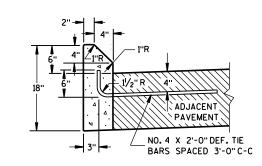
BARS SPACED 3'-0" C-C

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-O" BEHIND THE BACK OF CURBS.

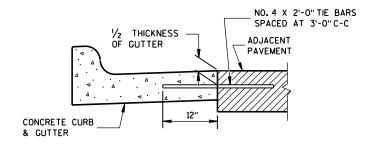
- 1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A.G.K.R AND TBTT.
- 2 THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- (9) REFER TO SDD 8D18 AND SDD 8D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



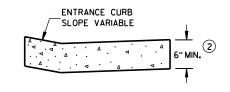
TYPES A D

TYPES G 4 J

CONCRETE CURB



TYPICAL TIE BAR LOCATION 1



DRIVEWAY ENTRANCE CURB (9)

(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

/S/ Rodney Taylor June, 2017 DATE

ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

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TYPICAL APPLICATION OF SILT FENCE

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PLAN VIEW SILT FENCE AT MEDIAN SURFACE DRAINS



GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- \bigcirc HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- 3 WOOD POSTS SHALL BE A MINIMUM SIZE OF 11/8" X 11/8" OF OAK OR HICKORY.
- 4) SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- (5) CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Cannestra

29-05 /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER

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INLET PROTECTION, TYPE A

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- 1) FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- (2) FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- (3) FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE, THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION TYPE A, B, C, AND D

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

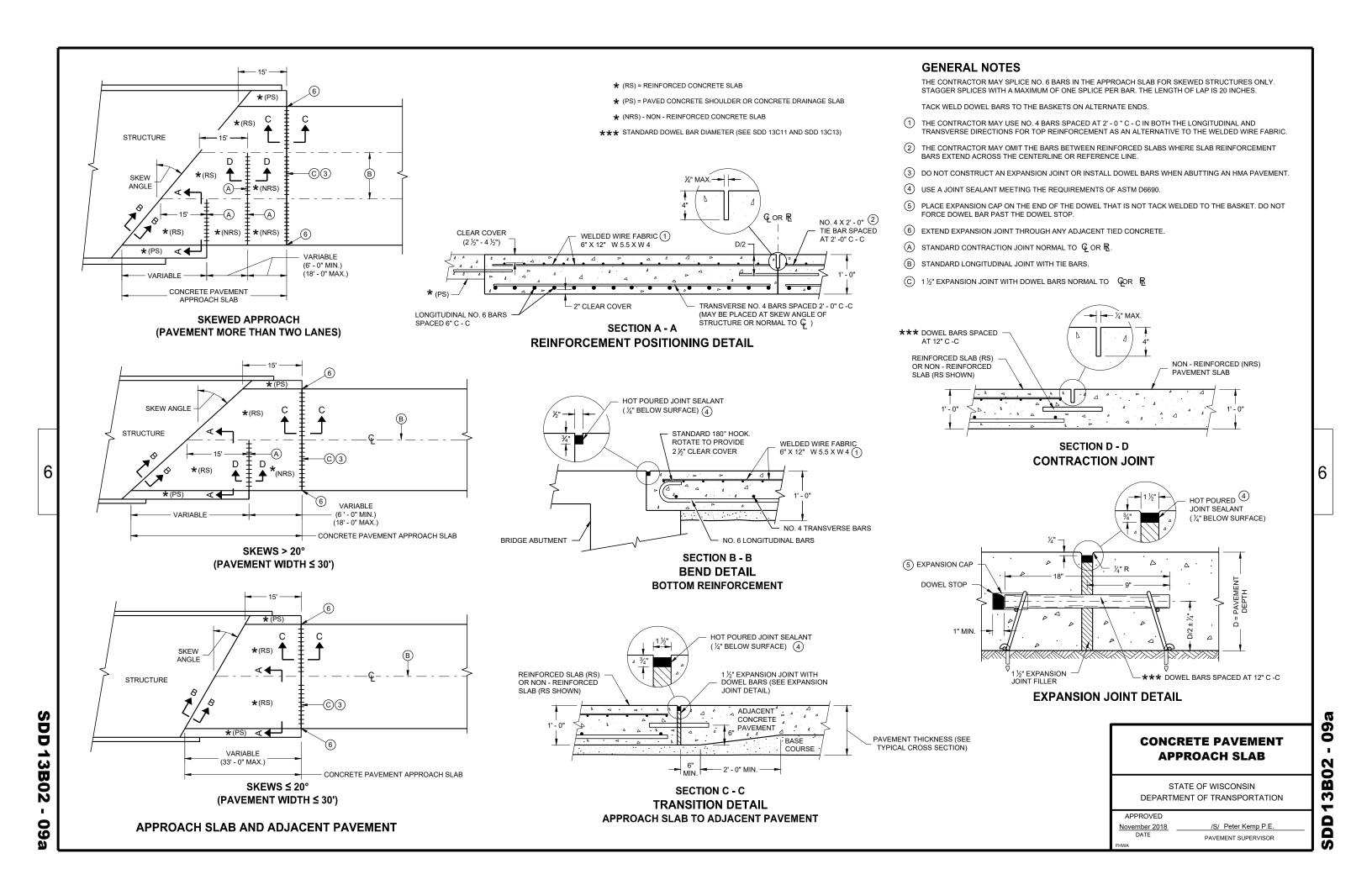
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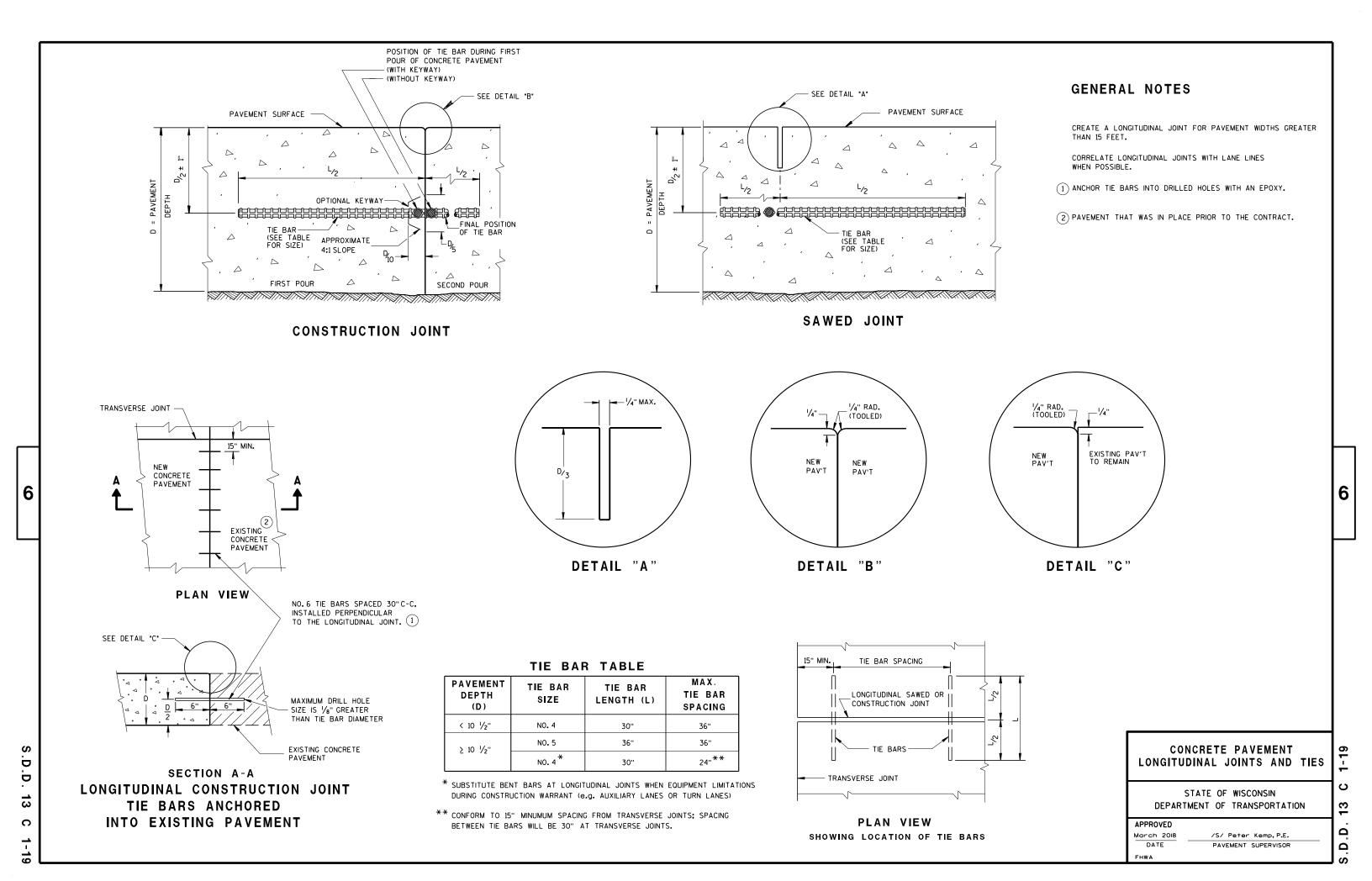
/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER 6

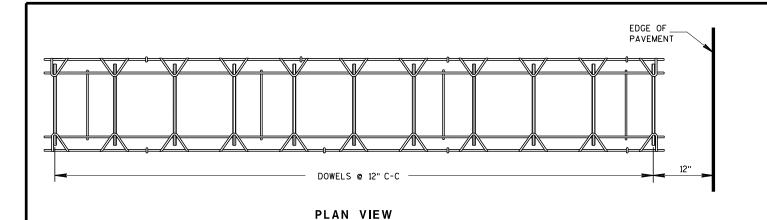
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PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING			
5 ½", 6",6 ½"	NONE	12'			
7",7 1/2"	1"	14'			
8",8 1/2"	1 1/4"	15'			
9",9 1/2"	1 1/4"	15'			
10" & ABOVE	1 1/2"	15'			

GENERAL NOTES

CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE LONGITUDINAL JOINT AND THE FREE EDGE OF PAVEMENT.

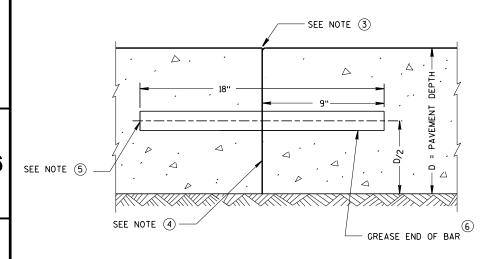
CONSTRUCTION JOINTS

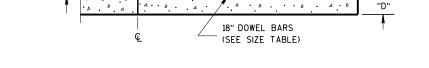
LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.

- (1) OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.
- ② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT LIPON FIELD CONDITIONS
- (3) FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/4-INCH RADIUS AT FORMED JOINTS.
- 4 PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- (5) INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING.
 INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT
 EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF
 DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL
 BARS ACCORDING TO DRILLED DOWEL BAR CONSTRUCTION JOINT DETAIL.
- (6) APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- (7) ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS 1/8-INCH GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.

SIDE VIEW

CONTRACTION JOINT DOWEL ASSEMBLY





DRILLED DOWEL BAR CONSTRUCTION JOINT $^{\scriptsize \bigcirc}$

1'-3",1'-3" | 1'-3", 1'-3", 1'-3", 1'-3", 2'-0", 1'-3", 1'-3", 1'-3"

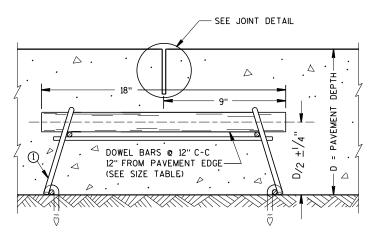
(FOR 11' LANE WIDTH REDUCE CENTER SPACE TO 1'-O")

PAVEMENT

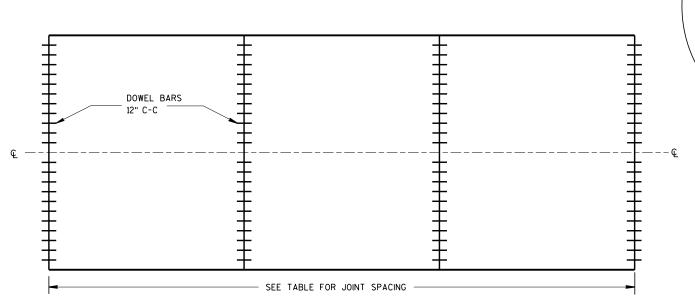
DEPTH

LANE WIDTH

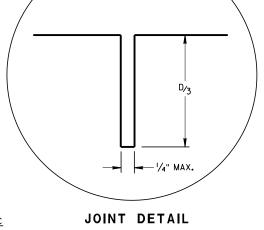
TRANSVERSE CONSTRUCTION JOINT



DOWELED CONTRACTION JOINT



CONTRACTION JOINT LOCATIONS



URBAN DOWELED CONCRETE PAVEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

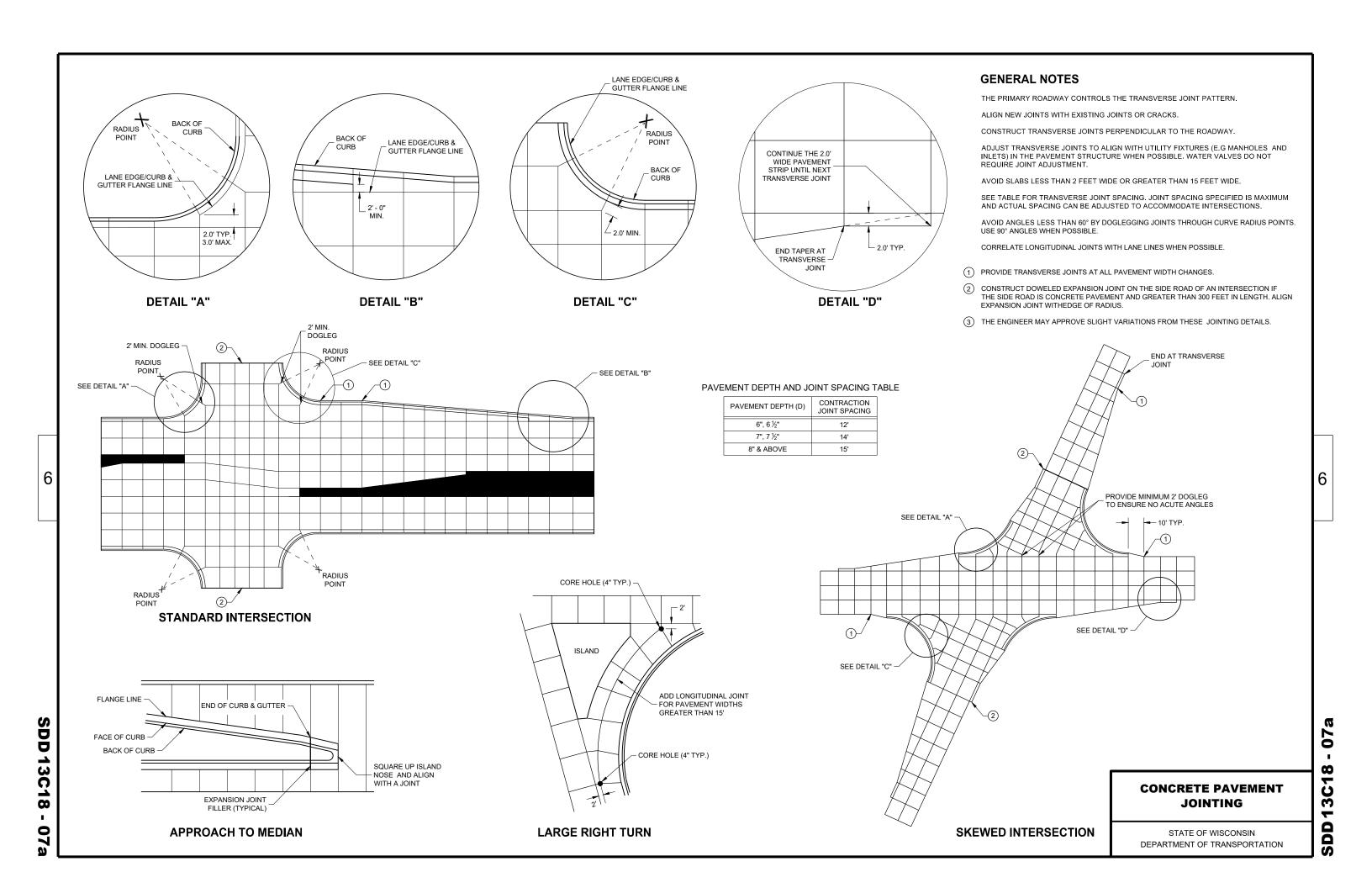
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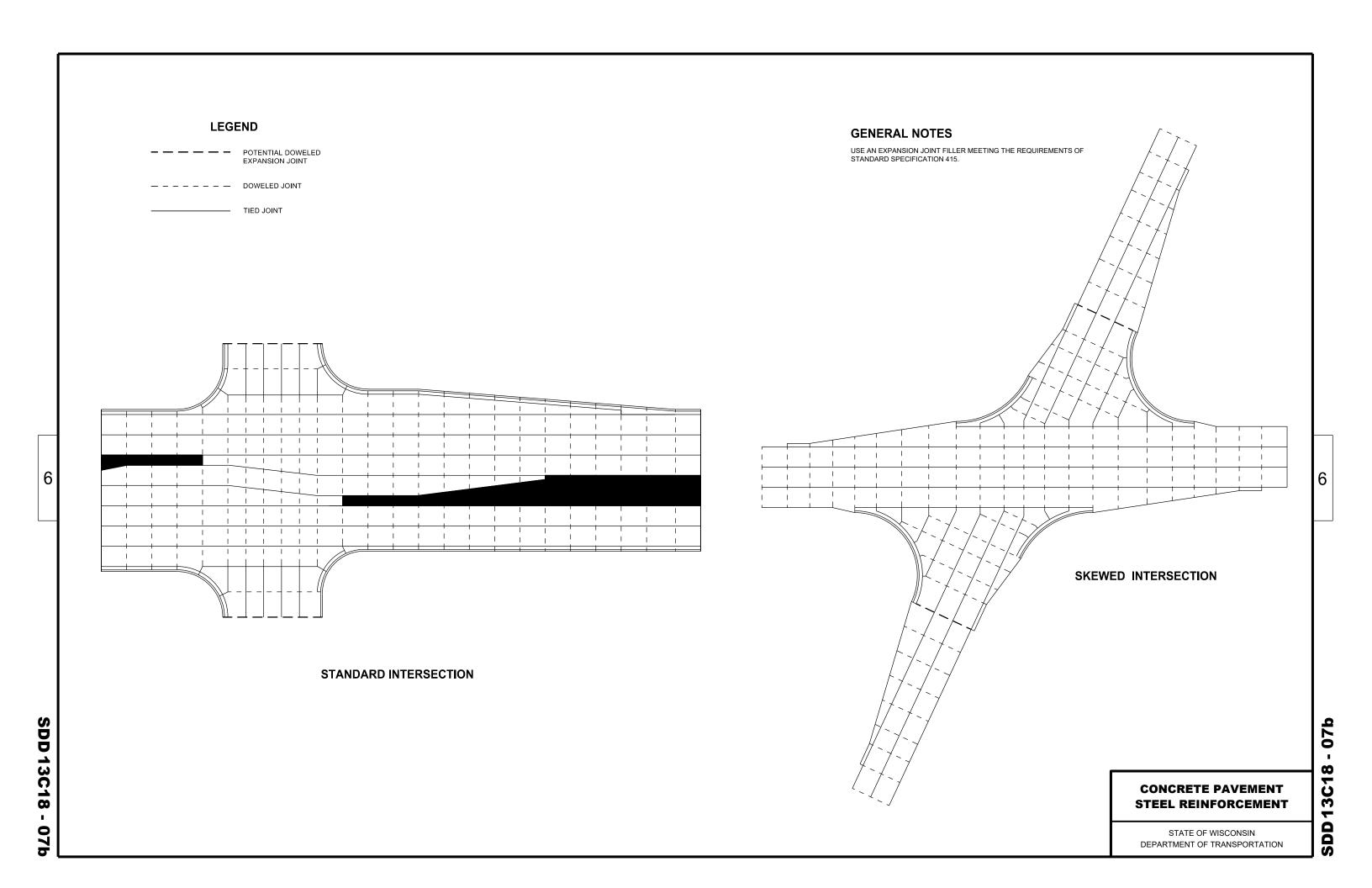
March 2018 /S

DATE PA

/S/ Peter Kemp, P.E.
PAVEMENT SUPERVISOR

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STATE OF WISCONSIN

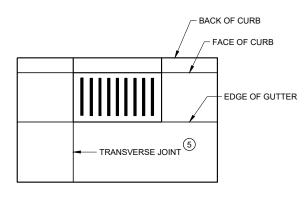
DEPARTMENT OF TRANSPORTATION

NO BOXOUT

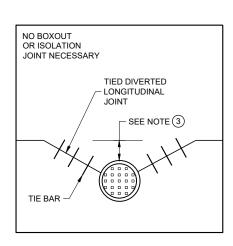
OR ISOLATION

JOINT NECESSARY

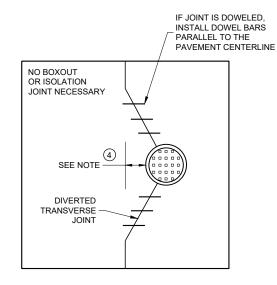
MANHOLE WITH TRANSVERSE JOINT



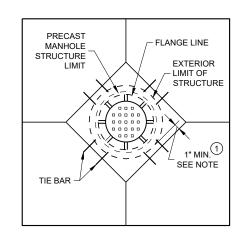
INLET WITH TRANSVERSE JOINT



MANHOLE WITH DIVERTED LONGITUDINAL CONTRACTION JOINT



MANHOLE WITH DIVERTED TRANSVERSE CONTRACTION JOINT



DIAGONAL MANHOLE BOXOUT FOR CONSTRUCTION JOINTS

GENERAL NOTES

- (1) USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1 FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.
- 2) ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.
- $\ensuremath{\mathfrak{J}}$ IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS, DIVERT THE LONGITUDINAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REINFORCEMENT REBAR AROUND THE MANHOLE.
- (4) IF THE DISTANCE FROM THE EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS LESS 4 FEET OR LESS, REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REINFORCEMENT REBAR AROUND THE MANHOLE.
- (5) ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.

CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES

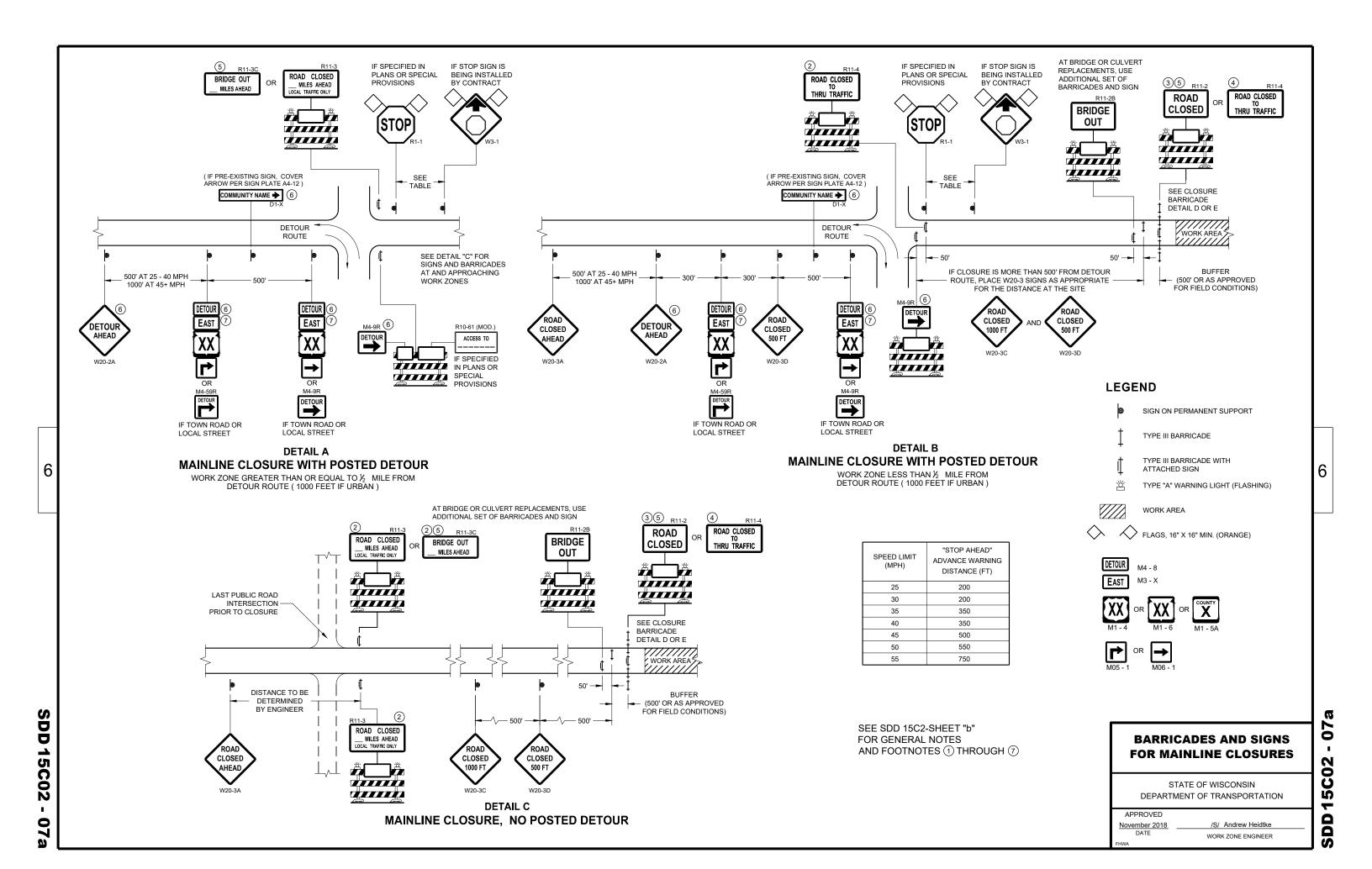
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

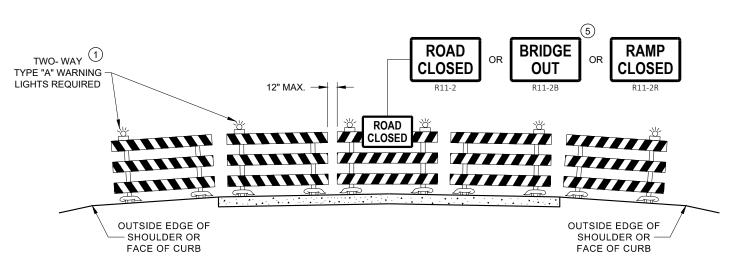
APPROVED

November 2018 DATE /S/ Peter Kemp P.E. PAVEMENT SUPERVISOR

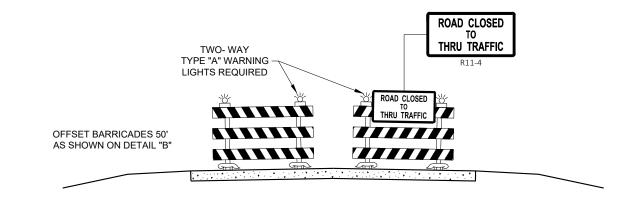
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DETAIL D ROAD CLOSURE BARRICADE DETAIL **APPROACH VIEW**



DETAIL E LANE CLOSURE BARRICADE DETAIL APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2. R11 - 3. M4 - 9. R11 - 4. AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11 - 2 SHALL BE 48" X 30"

R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"

M4 - 9 SHALL BE 30" X 24"

M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)

M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)

M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)

MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)

D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

R1 - 1 SHALL BE 36" X 36"

- TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT **SPACING**
- THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- (3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- (4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- (5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- (6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS. PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE
- "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR **VARIOUS CLOSURES**

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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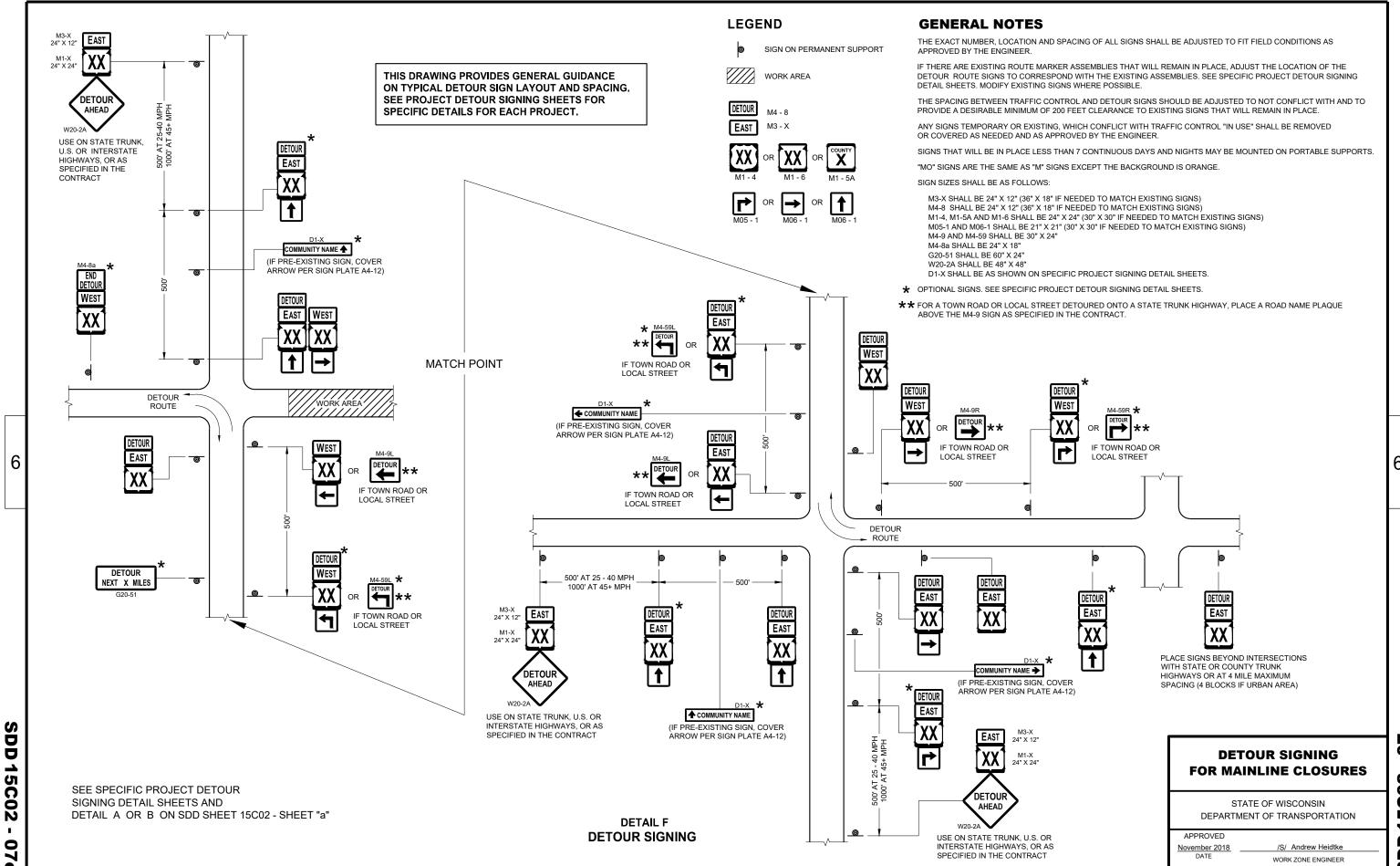
November 2018 DATE

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MAINLINE ROADWAY UNDER CONSTRUCTION

ROAD

WORK

AHEAD

CENTER OF ROADWAY

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48"

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

imes The Third W20-1Sign is required only if there is an intersection BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED 6

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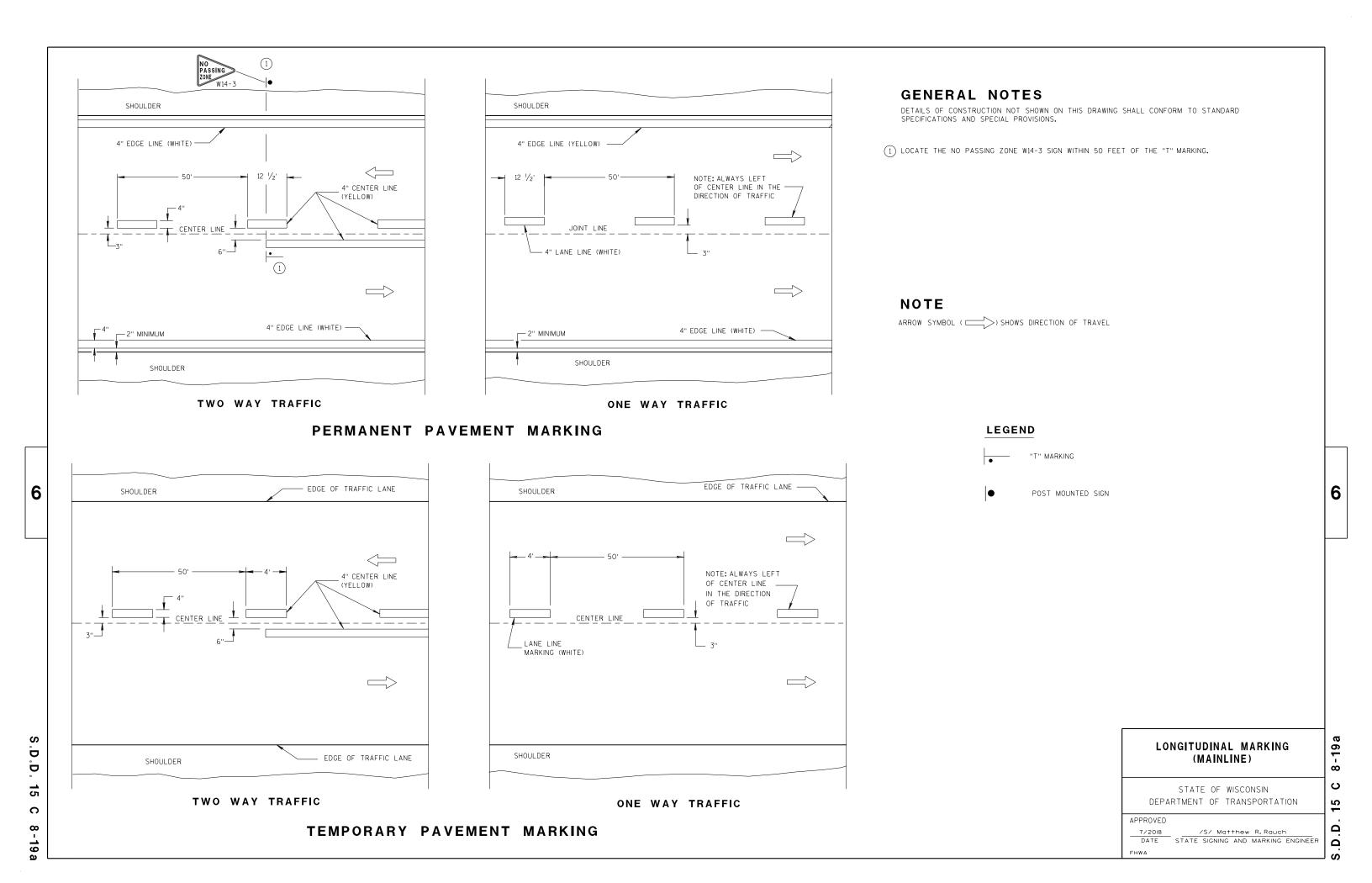
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

ROAD OPEN TO TRAFFIC

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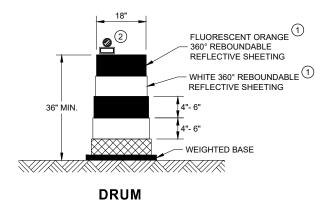
7/2018 /S/ Andrew Heidtke DATE WORK ZONE ENGINEER

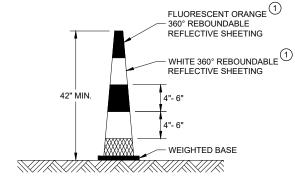
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GENERAL NOTES

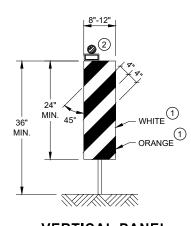
- (1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- (2) LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



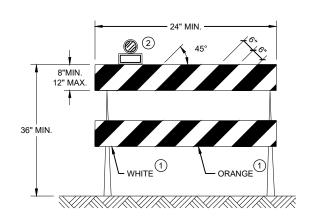


42" CONE DO NOT USE IN TAPERS

½ SPACING OF DRUMS

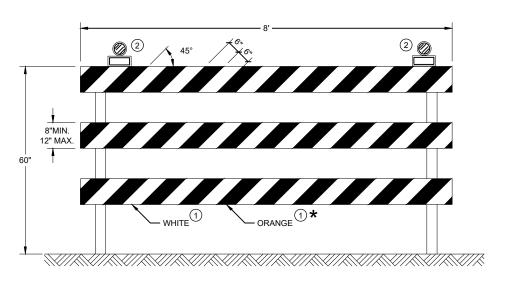


VERTICAL PANEL THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

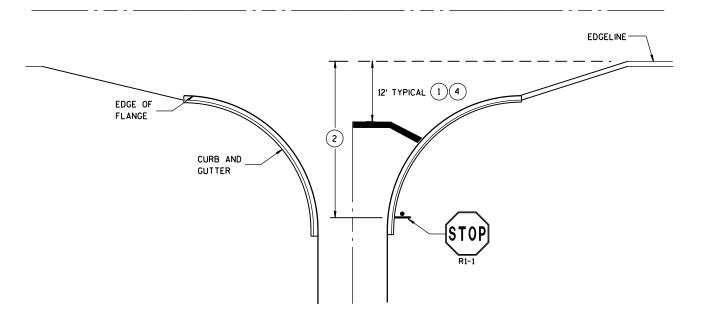
CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

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SDD 15C

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

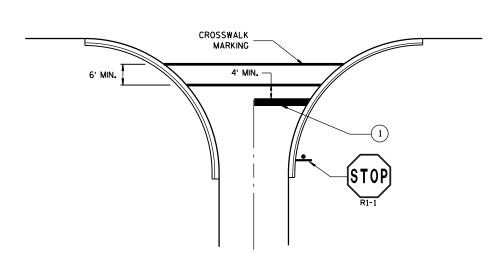
APPROVED	
June 2017	/S/ Andrew Heidtke
DATE	WORK ZONE ENGINEER
CLUAZA	



8" CHANNELIZATION WHITE FLANGELINE (EXTENSION) WHITE EDGELINE 4' TYPICAL (4)

TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



- EDGELINE 12' TYPICAL (1)

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

- 1 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- 2 IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
- (3) IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- (4) MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK **PAVEMENT MARKING**

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED						
Sept., 2017		/S/ I	Matth	ew R.	Ro	uch
DATE	STATE	SIGNIN	G AND	MARKI	NG	ENGINEER
FHWA						

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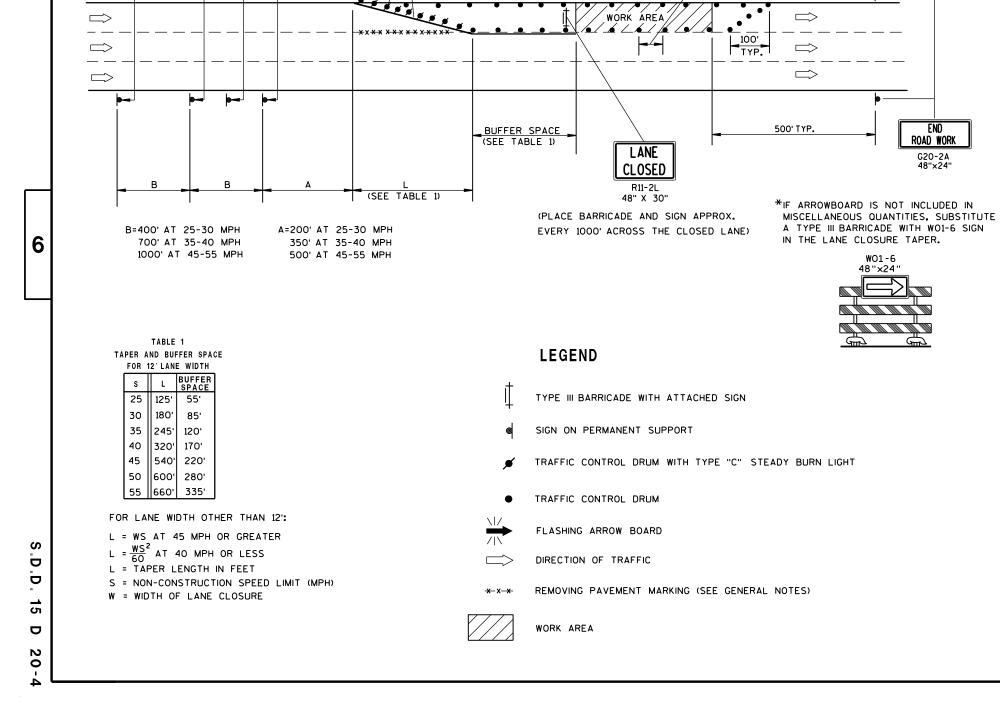
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(5) DRUMS SPACED @ 10'

INTERVALS AS NEEDED IN

FRONT OF ARROW BOARD

TEMPORARY PAVEMENT MARKING.

4-INCH REMOVABLE TAPE (WHITE ON RIGHT,

25'@ 35 MPH OR LESS 50'@ 40 MPH OR MORE

YELLOW ON LEFT)

SPACING:

ROAD WORK

NEXT___MILES

G20-1

60" X 24"

CLOSED

AHEAD

AHEAD

GENERAL NOTES

**THE LINE OF DRUMS SHOWN ALONG THE MEDIAN/CENTERLINE

ADJACENT TO THE WORK AREA. FOR THIS CONDITION INSTALL

W20-1 "ROAD WORK AHEAD" SIGN FOR OPPOSING DIRECTION OF

50' MAX. @ 35 MPH OR LESS

100' MAX. @ 40 MPH OR MORE

IS REQUIRED ONLY WHERE THERE IS OPPOSING TRAFFIC

TRAFFIC. IN ADVANCE OF THE WORK AREA.

SPACING:

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY

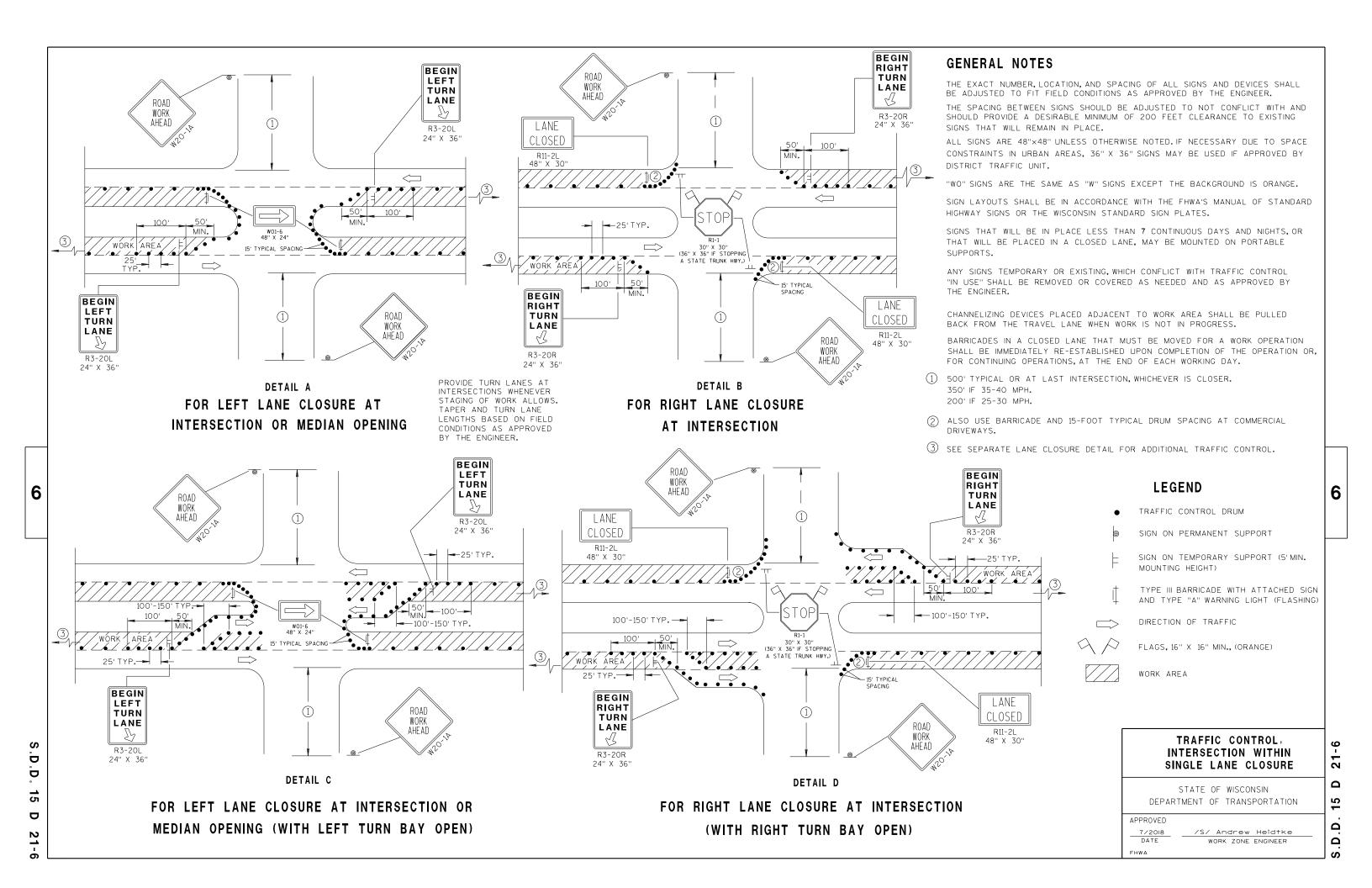
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED June 2016

/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

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TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SO. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SO.FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE). SIGNS LARGER THAN 27 SO.FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST **EMBEDMENT DEPTH**

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'

4" X 6" WOOD POST

POST SPACING REQUIREM	NUMBER OF		
L	E	WOOD POSTS REQUIRED	
48" OR LESS AND LESS THAN 20 SO.FT.	-	1	
LESS THAN 60"	12"	2	٤
60" TO 120"	L/5	2	
GREATER THAN 120" LESS THAN 168"	12"	3	
168" AND GREATER	12"	4	

SEE NOTE (3)

RURAL AREA

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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- 11/2" DIAMETER HOLES

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NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3"

MACHINE BOLTS - 1/6" X 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS

RIVETS - 32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

> ATTACHMENT OF SIGNS TO POSTS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017 /S/ Andrew Heidtke DATE WORK ZONE ENGINEER FHWA

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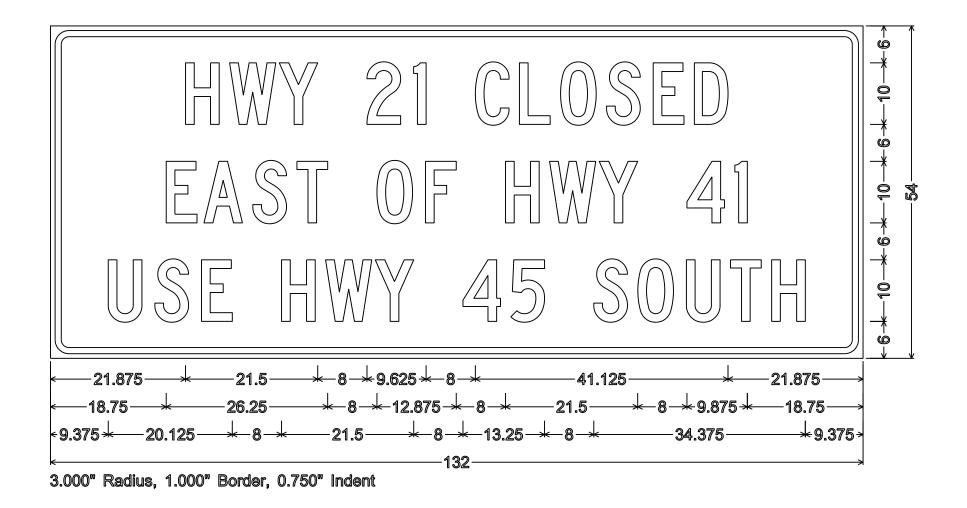
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38-2b

- 1. Fixed Message Type II Signs Type F Reflective
- 2. Color:

Background - Orange Message - Black

3. Message Series - C



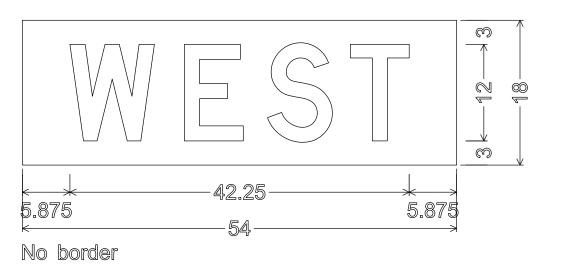
7

PROJECT NO: 6180-18-71 HWY: HWY 21 COUNTY: WINNEBAGO TEMPORARY SIGNING SHEET NO: E

- 1. Fixed Message Type II Signs Type F Reflective
- 2. Color:

Background - Orange Message - Black

- 3. Message Series D
- 4. Base Material .040" aluminum



PROJECT NO: 6180-18-71

HWY: STH 21

COUNTY: WINNEBAGO

TEMPORARY SIGNING

PLOT BY: mscj9h

SHEET NO:

Ε

FILE NAME: C:\CAEfiles\Projects\tr_d3_3701a319FMS.dgn

PLOT DATE : 27-MAR-2019 4:05

PLOT NAME :

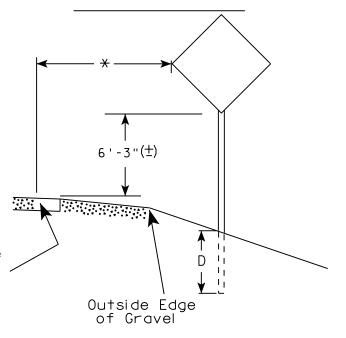
urban area

2' Min - 4' Max (See Note 6)

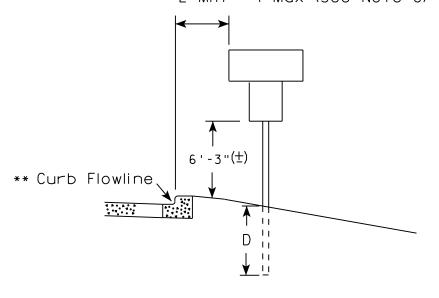
** Curb Flowline

D | White Edgeline Location

RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



White Edgeline
Location

Outside Edge
of Gravel

PLOT DATE: 21-AUG-2017 16:04

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated.

That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq.Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

GENERAL NOTES

- 1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is 7'- 3" (\pm) or 6'-3" (\pm) depending upon existence of a sub-sign.
- 4. J-Assemblies are considered to be one sign for mounting height.
- 5. Minimum mounting height for signs mounted on traffic signal poles is $5'-3''(\pm)$.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (\pm) tolerance for mounting height is 3 inches.
- 8. Folding signs shall be mounted at a height of 5'-3'' (\pm) or as directd by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch

For State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-3.21

SHEET NO:

PROJECT NO:

HWY:

COUNTY:

NTY:

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

PLOT SCALE : 100.601251:1.000000



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

GENERAL NOTES

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. J-Assemblies are considered to be one sign for mounting height.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3'' (\pm) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4''-3'' (±).
- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- ** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

D
(Min)
4'
5'

WISCONSIN DEPT OF TRANSPORTATION APPROVED For State Traffic Engineer DATE 8/21/17 PLATE NO. <u>A4-4.15</u>





	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRE)		
	L	E	
***	Greater than 48" Less than 60"	12"	
	60" to 108"	L/5	

HWY:

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 108" to 144"	12''

COUNTY:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 21-AUG-2017 15:54

PLOT SCALE: 108.188297:1.000000

WISDOT/CADDS SHEET 42

OF TYPE II SIGNS ON MULTIPLE POSTS

TYPICAL INSTALLATION

SHEET NO:

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:



Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Nather R Raw
For State Traffic Engineer

DATE <u>8/11/16</u>

PLATE NO. <u>44-8.8</u>

PROJECT NO:

FILE NAME : C:\CAFfiles\Projects\tr stdplote\A48 DCN

PLOT DATE . 11-416-2016 11:35

PINT RY * \$\$ nintuser \$\$

SHEET NO:

| | |



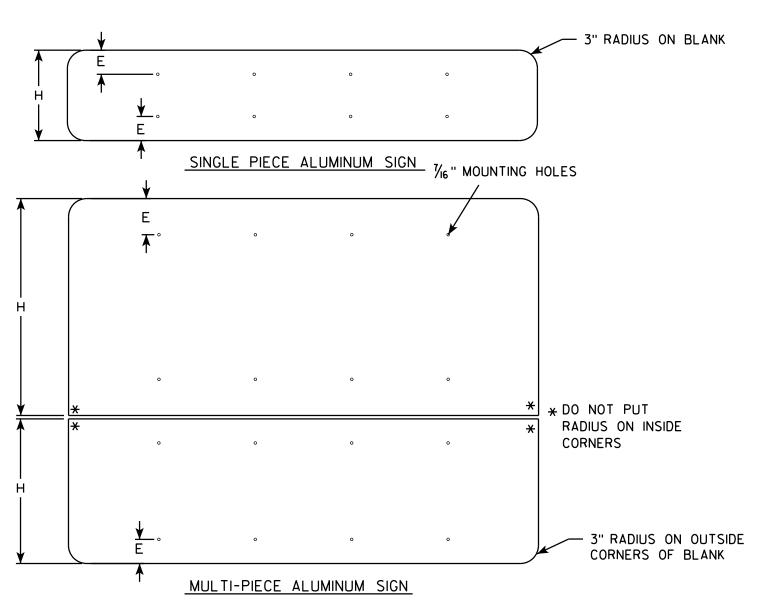
PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

For State Traffic Engineer

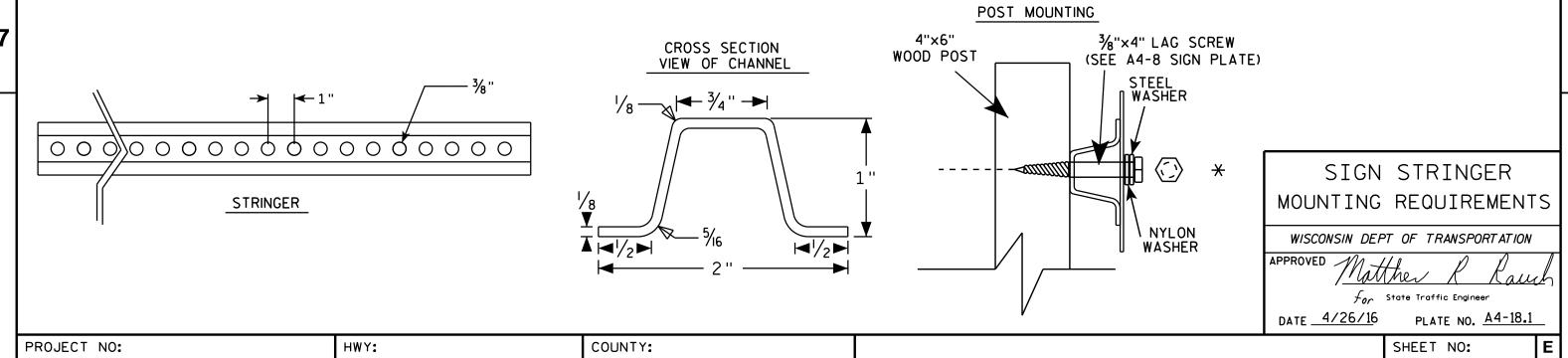




GENERAL NOTES

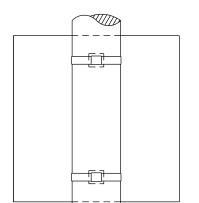
- ALL SIGNS OVER 60" IN WIDTH SHALL HAVE A 3" RADIUS ON THE OUTSIDE CORNERS OF THE ALUMINUM BLANK.
- MOUNTING HOLES SHALL BE $\frac{7}{16}$ " DIAMETER.
- SEE CHART FOR HOLE SPACING REQUIREMENTS
- FOR SIGN PANELS WITH DIMENSION (H) 36" AND OVER, DIMENSION E SHALL BE 6"
- FOR SIGN PANELS WITH DIMENSION (H) UNDER 36", DIMENSION E SHALL BE 4"
- SIGN STRINGER MATERIAL SHALL CONSIST OF STEEL CHANNEL POST SECTIONS, WEIGHING 1.12 LBS/FT IN ACCORDANCE WITH SECTION 633.2.1 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- SEE SIGN PLATE A4-8 FOR SIGN STRINGER BOLTING REQUIREMENTS.

SIGN WIDTH	STRINGER WIDTH	POSTS	HOLE SPACING				NTING OLES			
78"	72"	2	16''	15''	31''	47''	63"			
84''	72"	2	17''	161/2"	331/2"	501/2"	6 7 1/21			
90"	7 2"	2	18''	18''	36''	54''	72''			
96"	90"	2	19"	191/2"	381/2''	571/2"	761/21			
102"	90"	2	20"	21''	41''	61''	81''			
108''	90"	2	21''	221/21	' 43 ^l / ₂ ''	641/2"	851/21	1		
114''	108''	3	15''	12''	2 7 ''	42"	5 7 "	7 2"	87"	102"
120''	108''	3	16''	12''	28''	44''	60"	76"	92"	108''
126"	108''	3	17''	12''	29"	46''	63"	80"	97"	114''
132"	126''	3	18''	12''	30"	48"	66"	84"	102"	120''
138''	126''	3	19''	12''	31''	50"	69"	88"	107''	126"
144''	126''	3	20"	12''	32"	52"	72"	92"	112''	132"

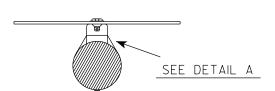


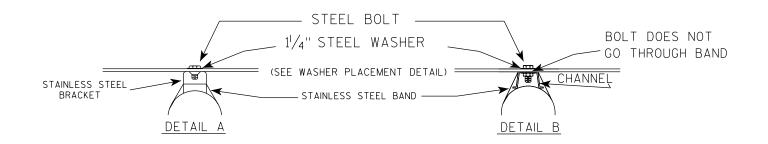
PLOT BY: mscj9h

BANDING

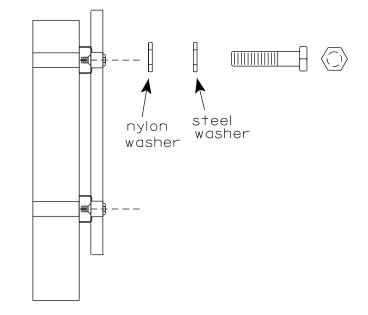


SINGLE SIGN





WASHER PLACEMENT



HWY:

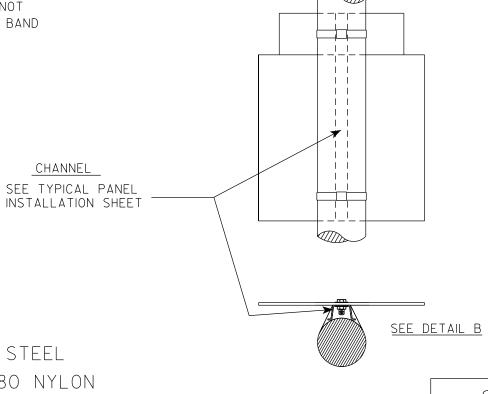
WASHERS (ALL POSTS) -

1-1/4" O.D. X³/₈" I.D. X¹/₁₆" STEEL 1-1/4" O.D. $\times \frac{3}{8}$ " I.D. \times .080 NYLON FOR ALL TYPE H SIGNS

GENERAL NOTES

- 1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
- 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
- 3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
- 4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



STANDARD SIGN SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

State Traffic Engineer

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APPROVED

DATE 6/10/19 PLATE NO. A5-9.4

COUNTY:

PLOT DATE: 10-JUN 2019 4:10

PLOT NAME :

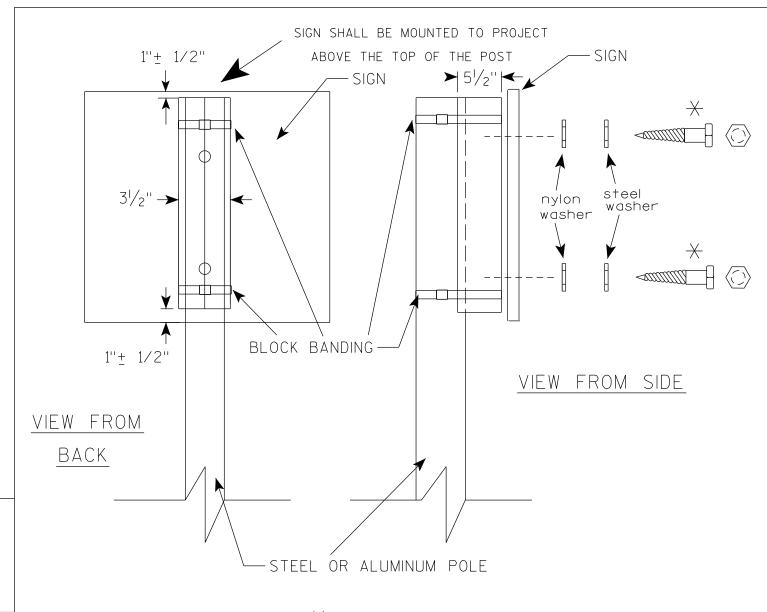
PLOT SCALE: \$\$.....plotscale.....\$\$ WISDOT/CADDS SHEET 42

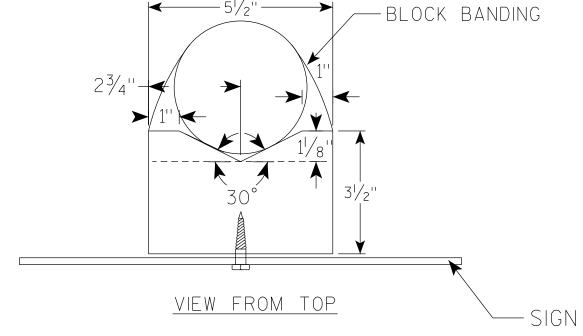
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn

PROJECT NO:

PLOT BY: mscj9h

CHANNEL





GENERAL NOTES

- 1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
- 2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
- 3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS.

 SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
- 4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
- 5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation: B 633, TYPE III, SC 3
- 6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
- 7. STEEL WASHERS SHALL BE $1\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
- 8. NYLON WASHERS SHALL BE $1^{1}/_{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

 \rightarrow LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL (V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

| APPROVED

For State Traffic Engineer

SHEET NO:

Matthew R

DATE 6/10/19

PLATE NO. _A5-10.2

PROJECT NO:

FILE NAME: C:\CAEfiles\Projects\tr_stdplate\A510.dgn

PLOT DATE: 10-JUN 2019 4:15

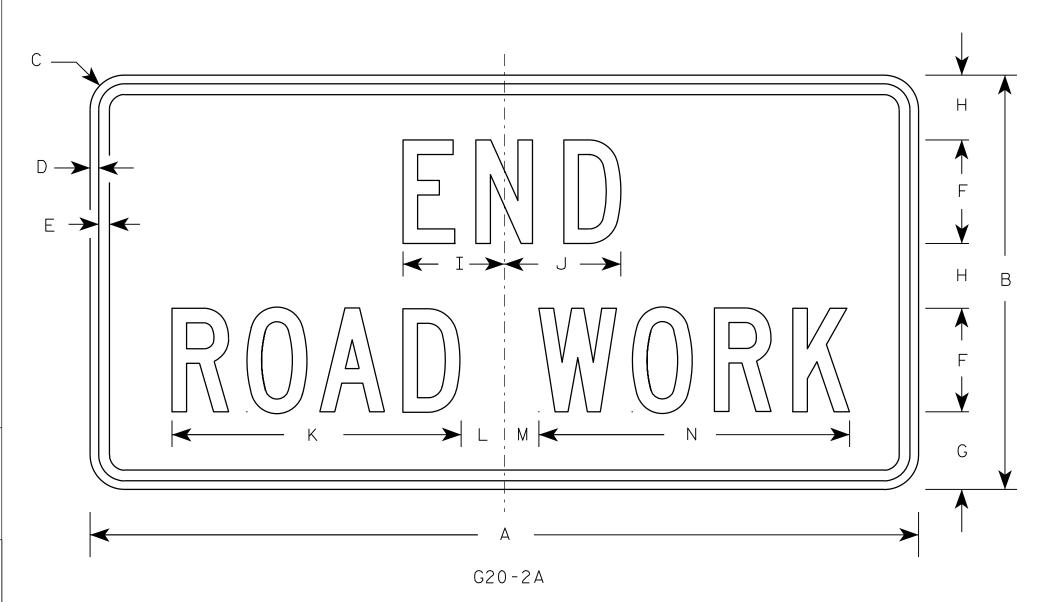
PLOT BY: mscj9h

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Metric equivalent for this sign is:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 1/8	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72

COUNTY:

STANDARD SIGN G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED 400 110 00 00 110

for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\G202A.DGN

HWY:

PROJECT NO:

PLOT DATE: 30-SEP-2009 09:31

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE : 5.561773:1.000000

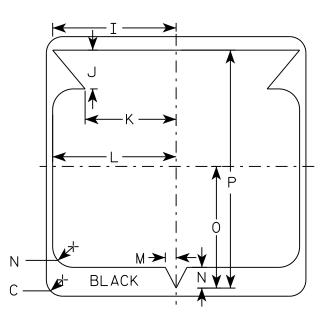
5.561773:1.000000 WISDOT/CADDS SHEET 42

- 1. Sign is Type II Type H Reflective
- 2. Color:

Background - White Message - Black

- 3. Message Series D except 3 number signs Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

	G F A H H
A A	
M1-6	1



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	Q	R	S	Т	U	٧	W	Х	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 1/8	11 1/2	1	1 1/8	11 1/4	21 1/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 ¾	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0

COUNTY:

STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

for State Traffic Engineer

DATE 3/16/18

PLATE NO. <u>M1-6.10</u>

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M16.DGN

HWY:

PROJECT NO:

PLOT DATE: 16-MAR-2018 14:11

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

PLOT SCALE : 6.655277:1.000000







MP3-1









HWY:



NOTES

- 1. All Signs Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

5. M3-1 thru M3-4 Background - White Message - Black

MB3-1 thru MB3-4 Background - Blue

Message - White

MK3-1 thru MK3-4 Background - Green

Message - White

MM3-1 thru MM3-4 Background - White

Message - Green

MN3-1 thru MN3-4 Background - Brown

Message - White

MP3-1 thru MP3-4 Background - White

Message - Blue

6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 1/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

COUNTY:

STANDARD SIGNS M3-1 thur M3-4 **SERIES**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 10/15/15 PLATE NO. M3-1.14

Ε

SHEET NO:

FILE NAME · C·\CAFfiles\Projects\tr stdolote\M31 DCN

PROJECT NO:

PLOT DATE . 01-DEC-2015 17:54

PLOT RY . \$\$ plotuser \$\$ PLOT NAME :

PLOT SCALE . 11 675051.1 000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

) A G	
	;
→ G →	
Y	

Α С E F G H I J S Х Z D 0 10 10 1/4 1 1/8 3/8 3/8 24 2.0 3 36 1 1/8 3/8 1/2 4 1/2 14 5/8 14 1/2 4.5 4 5

COUNTY:

STANDARD SIGN M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 11/10/10 PLATE NO. M4-8.2

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M48.DGN

PROJECT NO:

HWY:

PLOT DATE: 10-NOV-2010 13:18

PLOT BY : ditjph

PLOT SCALE : 4.767

PLOT NAME :

PLOT SCALE: 4.767233:1.000000

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

2. Color:

Background - Orange Message - Black

- 3. Message Series B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

 $D \longrightarrow$ Н M4-8A

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	U	٧	w	Х	Y	Z	Area sq. ft.
$\parallel 1 \parallel$																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5				·	·						·				·												

COUNTY:

STANDARD SIGN M4-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther

For State Traffic Engineer DATE 3/9/11

PLATE NO. M4-8A.2

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M48A.DGN

HWY:

PROJECT NO:

PLOT DATE: 09-MAR-2011 10:29

PLOT BY: mscj9h

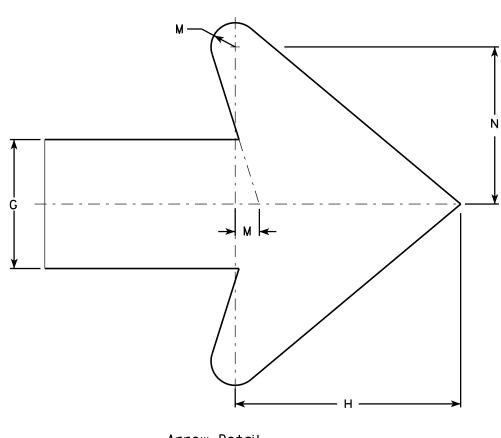
PLOT NAME :

PLOT SCALE: 3.972696:1.000000

- Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

PLOT NAME :

w x	Y Z Ar
	5.0
	12.
	12.

COUNTY:

M4-9R

STANDARD SIGN M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R

For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M49R.DGN

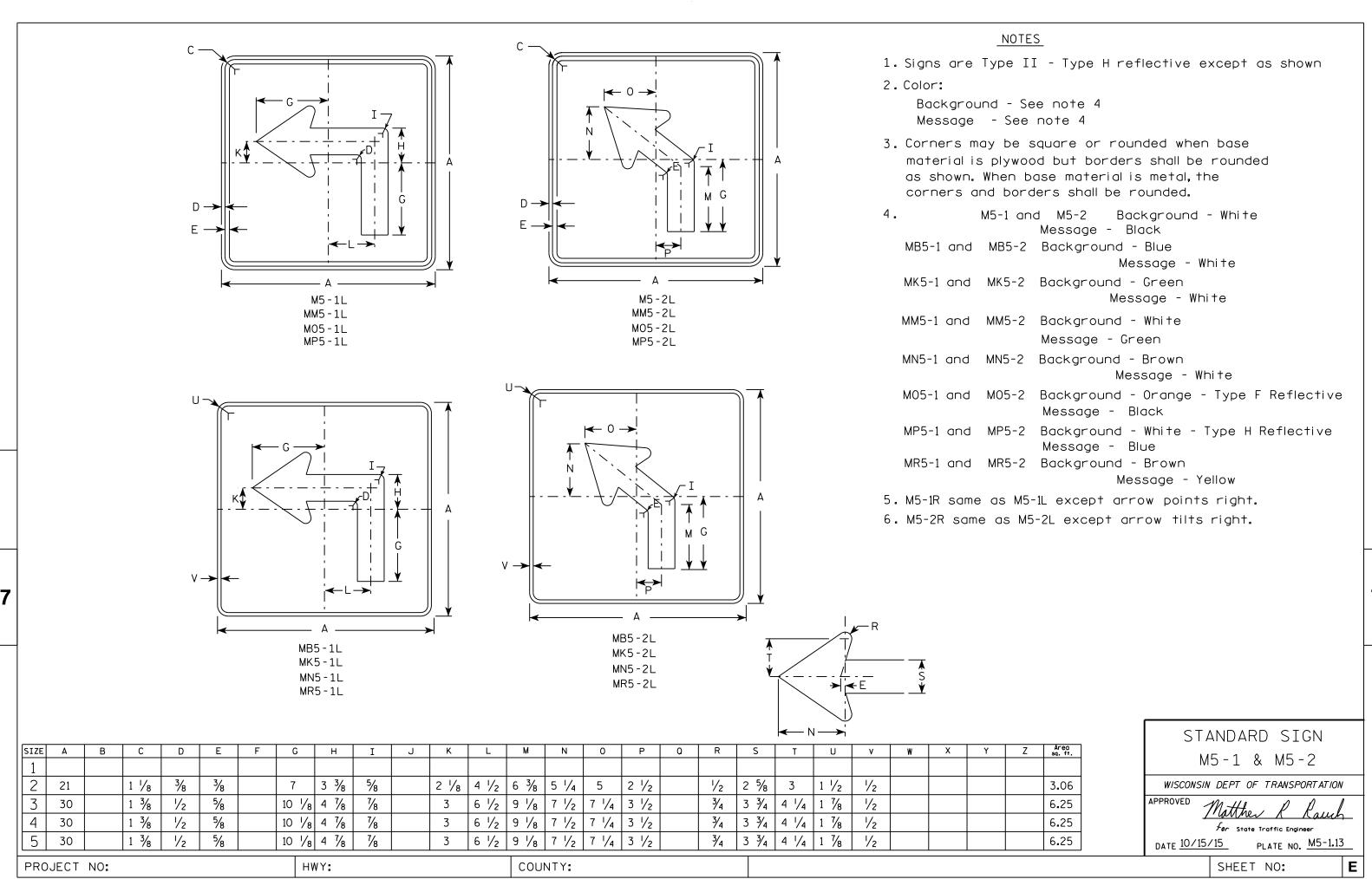
HWY:

PROJECT NO:

PLOT DATE: 09-MAR-2011 11:17

PLOT BY: mscj9h

PLOT SCALE: 5.959043:1.000000



FILE NAME . C.\CAFfiles\Projects\tr stdolote\M51 DCN

PLOT DATE . 01-DEC-2015 18:07

PINT RY . \$\$ DIOTUSET \$\$ PINT NAMF :

PLOT SCALE . 11 675051.1 000000







MR6-1

HWY:



NOTES

- 1. Signs are Type II Type H except as Shown
- 2. Color:

Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background White

Message - Black

MB6-1 and MB6-2 Background - Blue

Message - White

MK6-1 and MK6-2 Background - Green

Message - White

MM6-1 and MM6-2 Background - White

Message - Green

MN6-1 and MN6-2 Background - Brown

Message - White

M06-1 and M06-2 Background - Orange - Type F Reflective

Message - Black

MP6-1 and MP6-2 Background - White

Message - Blue

MR6-1 and MR6-2 Background - Brown

Message - Yellow



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	٥	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1 1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 %	5	4 1/4	5 1/4	3	2 %	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew & Rawl For State Traffic Engineer

Ε

DATE 10/15/15 PLATE NO. M6-1.15

SHEET NO:

FILE NAME · C·\CAFfiles\Projects\tr stdplote\M61 DCN

PROJECT NO:

PLOT DATE . 01-DEC-2015 17:57

PIOT RY . \$\$ plotuser \$\$ PIOT NAMF :

PLOT SCALE . 11 675051.1 000000

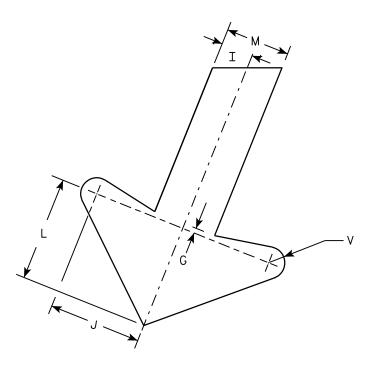
R3-20R

HWY:

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW	DETAIL

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	Q	R	S	Т	U	٧	W	Х	Y	Z	Area sq. ft.
1																											
25	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 1/8	2 %	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 1/8	2 %	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 %	8	22°	1/2	9 1/2				6.0
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 1/8	3	2 1/4	12 3/4	12 1/2		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

COUNTY:

STANDARD SIGN R3-20R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
Forstate Traffic Engineer

SHEET NO:

DATE 10/18/10

PLATE NO. <u>R3-20R.</u>6

М

Ν

PLOT DATE: 15-OCT-2010 14:59 PLOT BY: do+sja

PLOT NAME :

PLOT SCALE : 5.959043:1.000000

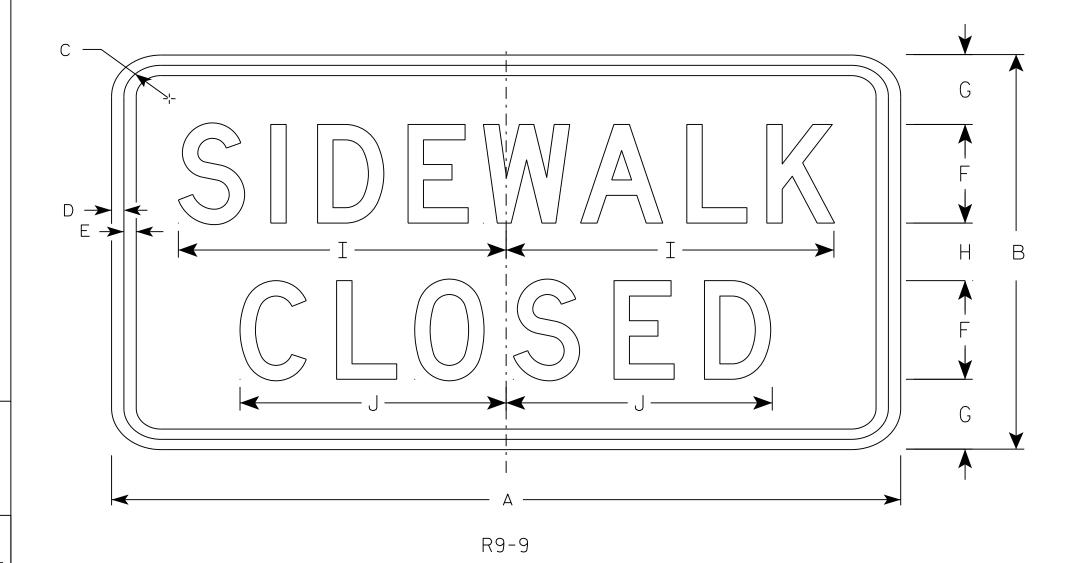
WISDOT/CADDS SHEET 42

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R320R.DGN

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



SIZE A 2S 24 1 3/4 1/2 2 1/8 1 3/4 10 1/2 12 3 8 1/8 2.0 24 1 3/4 1/2 2 1/8 1 3/4 8 1/8 12 10 2.0 1 3/4 3 1/2 30 18 1/2 1/2 3 | 12 1/2 | 10 1/4 3.75

COUNTY:

STANDARD SIGN R9-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

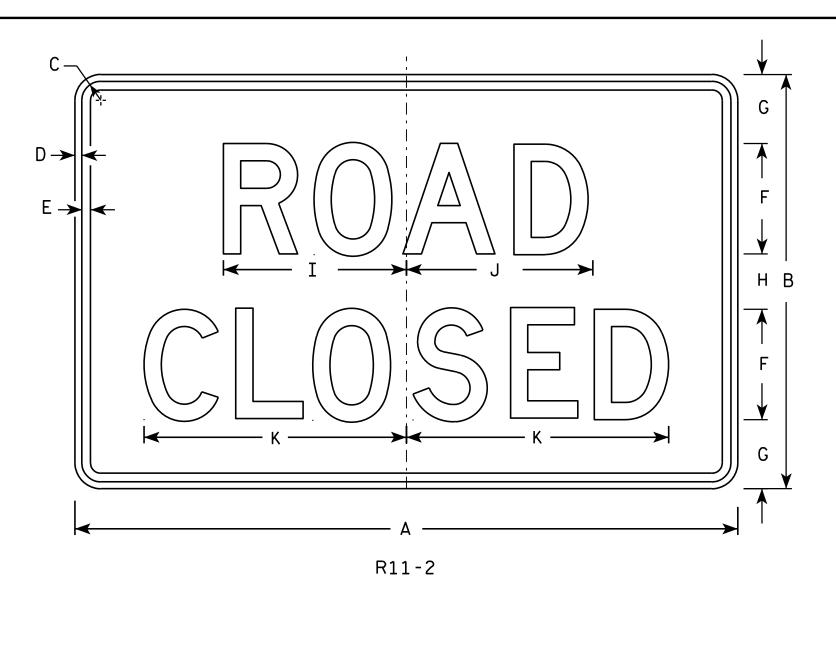
Marther R Ray

DATE <u>8/11/16</u>

SHEET NO: R9-9.6

Ε

HWY:

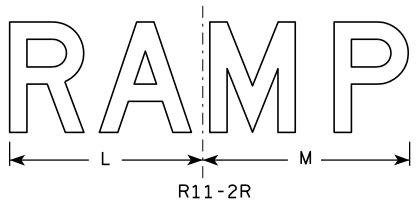


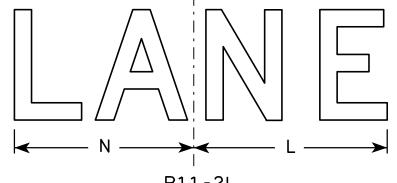
<u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Modify the message as required.





R	1	1	-	2	L

PLOT NAME :

SIZ	Έ	A	В	С	D	Ε	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	v	W	X	Y	Z	Area sq. ft.
1																												
2	S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
21	I	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 ½	19	14	15	13													10.0
3		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
4		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
5		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 ½	19	14	15	13													10.0

COUNTY:

STANDARD SIGN R11-2

WISCONSIN DEPT OF TRANSPORTATION

DATE 4/1/11 PLATE NO. R11-2.10

SHEET NO:

HWY:

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

C —		\
D A E A		$ \begin{array}{c c} G & \hline & F & \hline & B & \hline & G & G & G & \hline & G & G & G & G & \hline & G & G & G & G & \hline & G & G & G & G & G & G \\ & G & G & G & G & G & G $
	R11-2B	

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	M	N	0	Р	0	R	S	T	U	V	W	X	Y	Z	Areg sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
2M	48	30	1 %	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
4	48	30	1 %	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0

STANDARD SIGN R11-2B

WISCONSIN DEPT OF TRANSPORTATION

DATE 4/1/11 PLATE NO. R11-2B.2

SHEET NO:



- 1. Sign is Type II Type H Reflective
- 2. Color:

G

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.

HWY:

R11-3B

** See Note 5

 $D \rightarrow$

E→

STANDARD SIGN R11-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

PLATE NO. R11-3B.3

SHEET NO:

SIZE A В С D 1 3/8 5/8 10 3/4 8 3/8 4 3/4 6 1/2 36 18 1/2 4 3 2 1/2 2 2 13 1/4 2 1/4 3 1 1/2 2 2 6 3/4 7 1/8 4.5 60 30 1 3/8 1/2 4 1/4 3 3/8 20 1/8 13 1/4 1 3/4 17 3/8 13 1/8 3 1/2 12.5 5 10 11 1/8 2M 4 1/4 3 3/8 20 1/8 13 1/4 1 3/4 3 1/2 11 1/8 30 17 3/8 13 1/8 12.5 4 5

COUNTY:

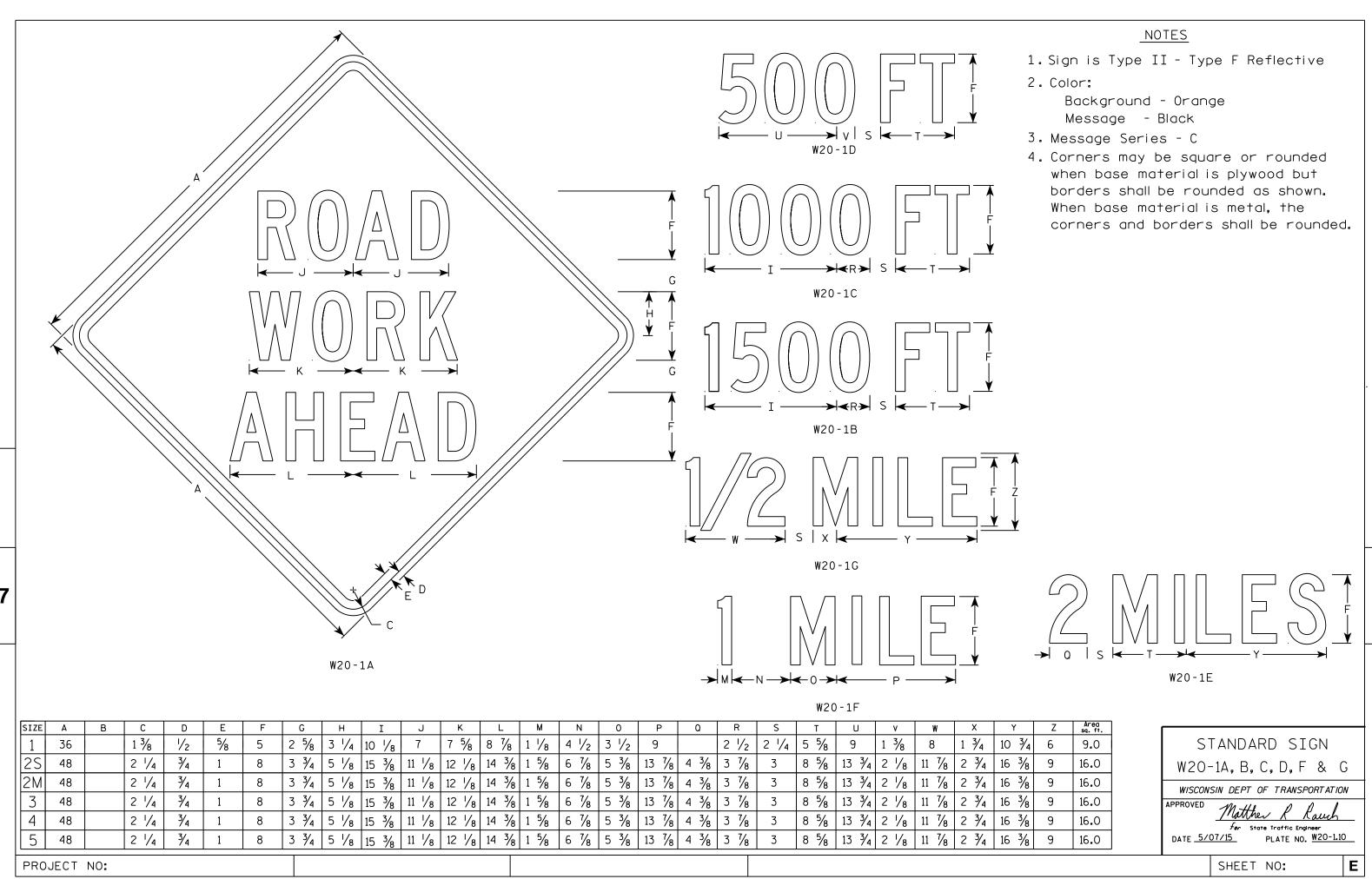
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\R113B.DGN

PROJECT NO:

PLOT DATE: 21-MAR-2017 08:46

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

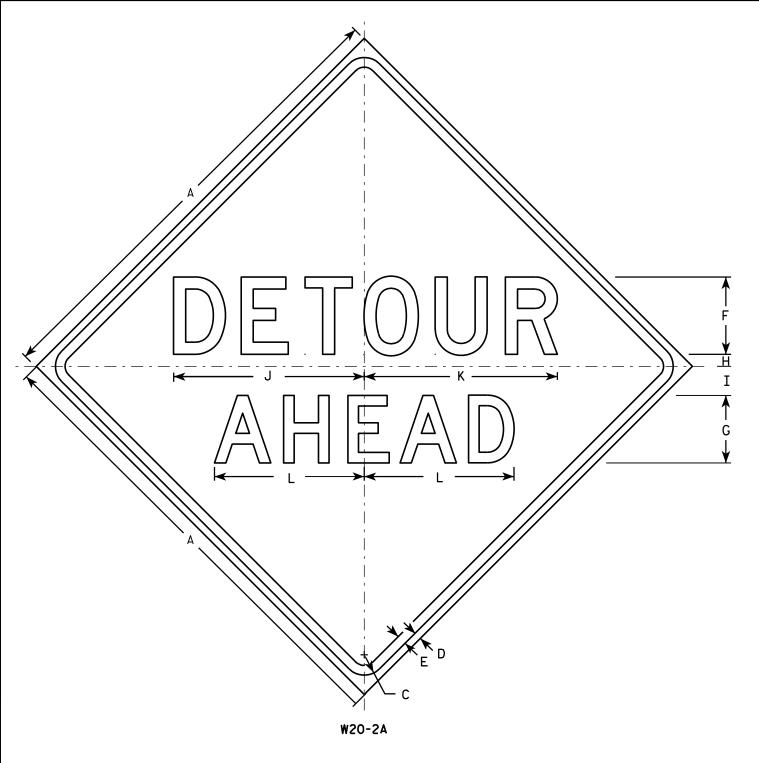
PLOT SCALE: 6.896672:1.000000



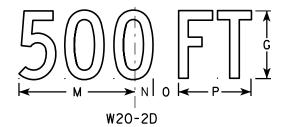
FILE NAME . C.\CAFfiles\Projects\tr stdolote\W201 DCN

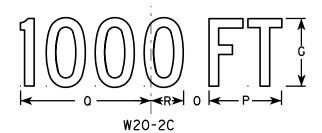
PLOT DATE . 01-DEC-2015 18.24

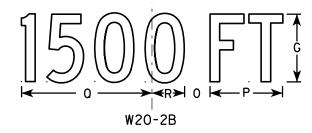
PIOT RY * \$\$ plotuser \$\$

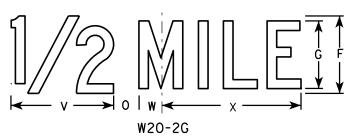


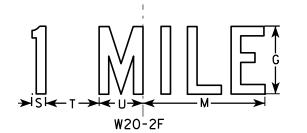
HWY:











PLOT BY: mscj9h

NOTES

- Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Line 1 is Series D.
 Line 2 is Series D for AHEAD and
 Series C for all other distances.

SIZE	. Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	36		1 1/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 1/8	5 %	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
3	48		2 1/4	₹4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
4	48		2 1/4	₹4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 %	2 3/8	14 3/8	·		16.0

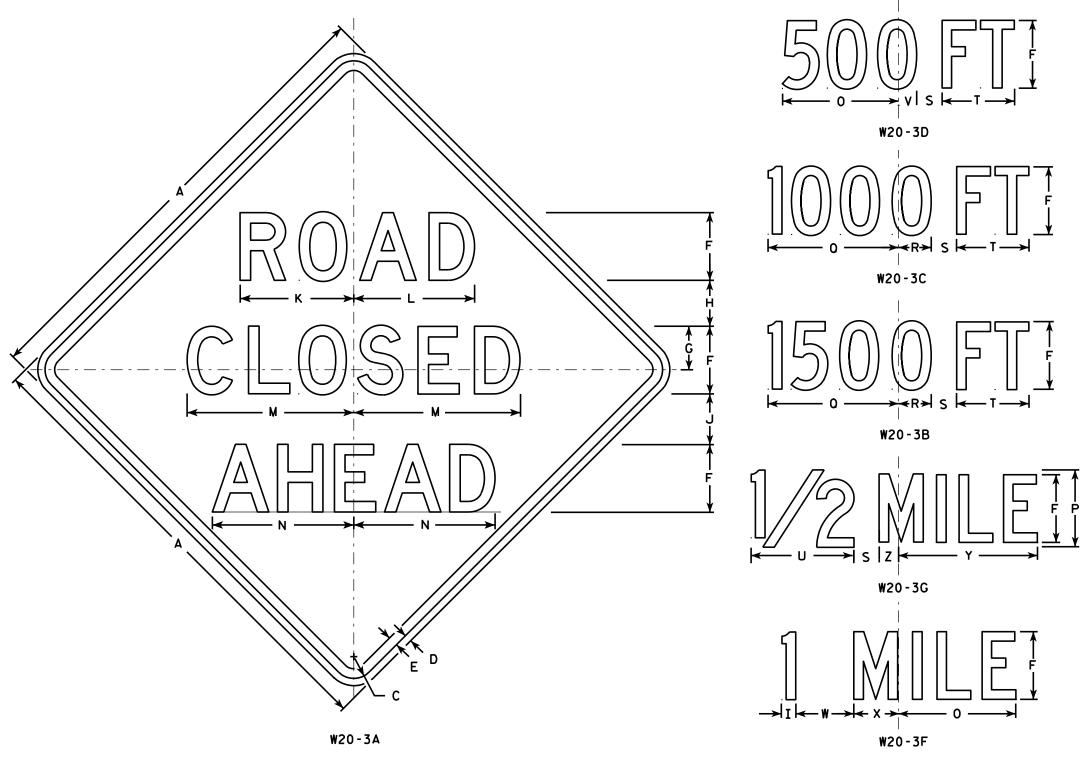
COUNTY:

STANDARD SIGN W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

DATE 3/18/11 PLATE NO. W20-2.6

SHEET NO:



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series see note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Lines 1 and 2 are Series D. Line 3 is Series D for AHEAD and Series C for all other distances.

1 % 5/8 ¾ 8 3/8 8 7/8 12 1/2 5 % 1 3/8 4 1/2 36 3 1/2 10 3/4 1 3/4 8 4 \(\frac{5}{8} \) 14 \(\frac{3}{8} \) 2 \(\frac{3}{8} \) 16.0 3/4 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 5/8 1 7/8 2M 3/4 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 48 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 % 1 % 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 3/4 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 4 % | 14 % | 2 % | 16.0 48 3/4 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 13 1/2 3 3/8 2 5/8 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 7 1/2 10 5/8 1 7/8 48 5 4 5/8 14 3/8 2 3/8 16.0 3/4 2 1/4 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 48

COUNTY:

STANDARD SIGN W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer DATE 3/18/11

PLATE NO. W20-3.7

SHEET NO:

PROJECT NO: FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN HWY:

PLOT DATE: 18-MAR-2011 12:08

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: 9.931739:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. " _____ LANE" is Series B. All other copy is Series C.

W20-5D

W20-5B

W20-5G

PLOT BY: mscj9h

->I0I← R-		
	W20-5F	

								W20-	5 A																	11 2	20-56
SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	v	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	7/8	2 1/2	13 1/8	10 ¾	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 1/8	5 %	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 %	7 1/2	13 1/2	3 3/8	2 3/8	10 %	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 %	7 1/2	13 1/2	3 %	2 3/8	10 %	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 %	1 1/2	6	4 %	12	2 %	7 1/2	13 1/2	3 %	2 3/8	10 %	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 %	1 1/2	6	4 5/8	12	2 %	7 1/2	13 1/2	3 %	2 3/8	10 %	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 %	7 1/2	13 1/2	3 3/8	2 3/8	10 %	16.0

COUNTY:

STANDARD SIGN W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Kauch Fer State Traffic Engineer DATE 3/18/11 PLATE NO. W20-5.11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W205.DGN

PROJECT NO:

HWY:

W20-56A

W20-55A

PLOT DATE: 18-MAR-2011 12:15

PLOT NAME :

PLOT SCALE: 11.918087:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

	G
	_ ¥ B
W01-6	

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	M	N	0	Р	0	R	S	Т	U	٧	W	Х	Y	Z	Areg sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 ¾													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 ¾													12.5

COUNTY:

STANDARD SIGN WO1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch
For State Traffic Engineer

For State Traffic Engineer

13 PLATE NO. <u>W01-6.1</u>

DATE <u>11/18/13</u>

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W016.DGN

HWY:

PROJECT NO:

PLOT DATE : 28-FEB-2014 11:37

PLOT NAME :

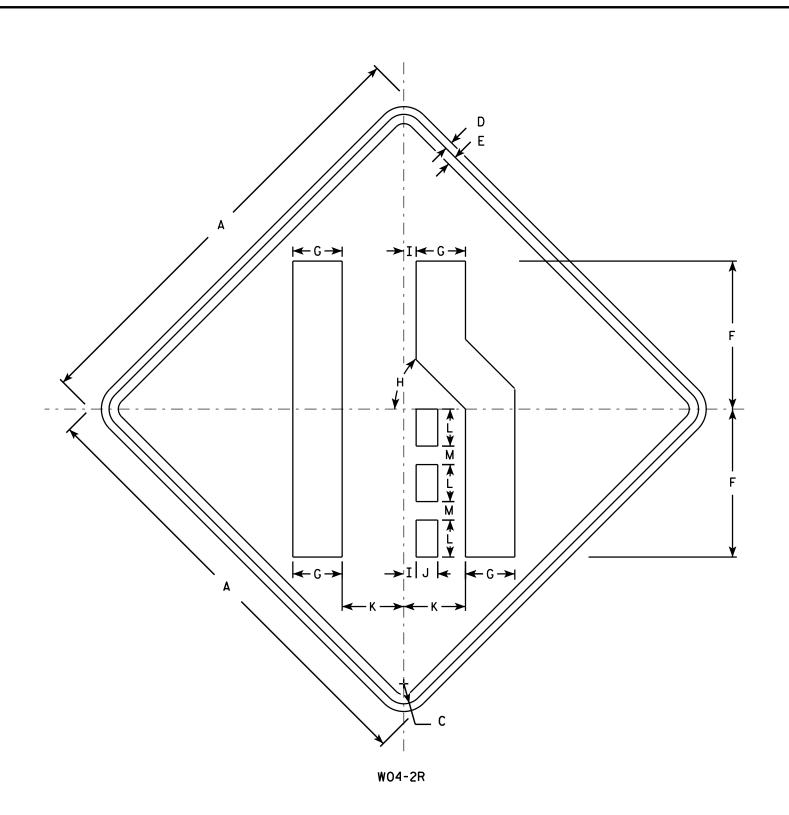
PLOT BY: mscj9h

PLOT SCALE: 5.837526:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W04-2L is the same as W04-2R except the symbolis reversed along the vertical centerline.



SIZE 1 % 5/8 3/4 12 45° 1 3/4 5 1 1/2 4 36 3 9.0 2S 2 1/4 5 3/8 45° 1 ¼ 2 ¾ 6 ¾ 3/4 48 16.0 45° 1 ¼ 2 ¾ 6 ¾ 3/4 5 3/8 48 2 1/4 2 16.0 2 1/4 3 48 3/4 5 % 45° | 1 1/4 | 2 3/8 | 6 3/4 2 16.0 2 1/4 3/4 5 3/8 45° | 1 1/4 | 2 3/8 | 6 3/4 48 2 16.0 5 2 1/4 3/4 5 3/8 45° | 1 1/4 | 2 3/8 | 6 3/4 48 2 16.0

STANDARD SIGN W04 - 2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

ForState Traffic Engineer

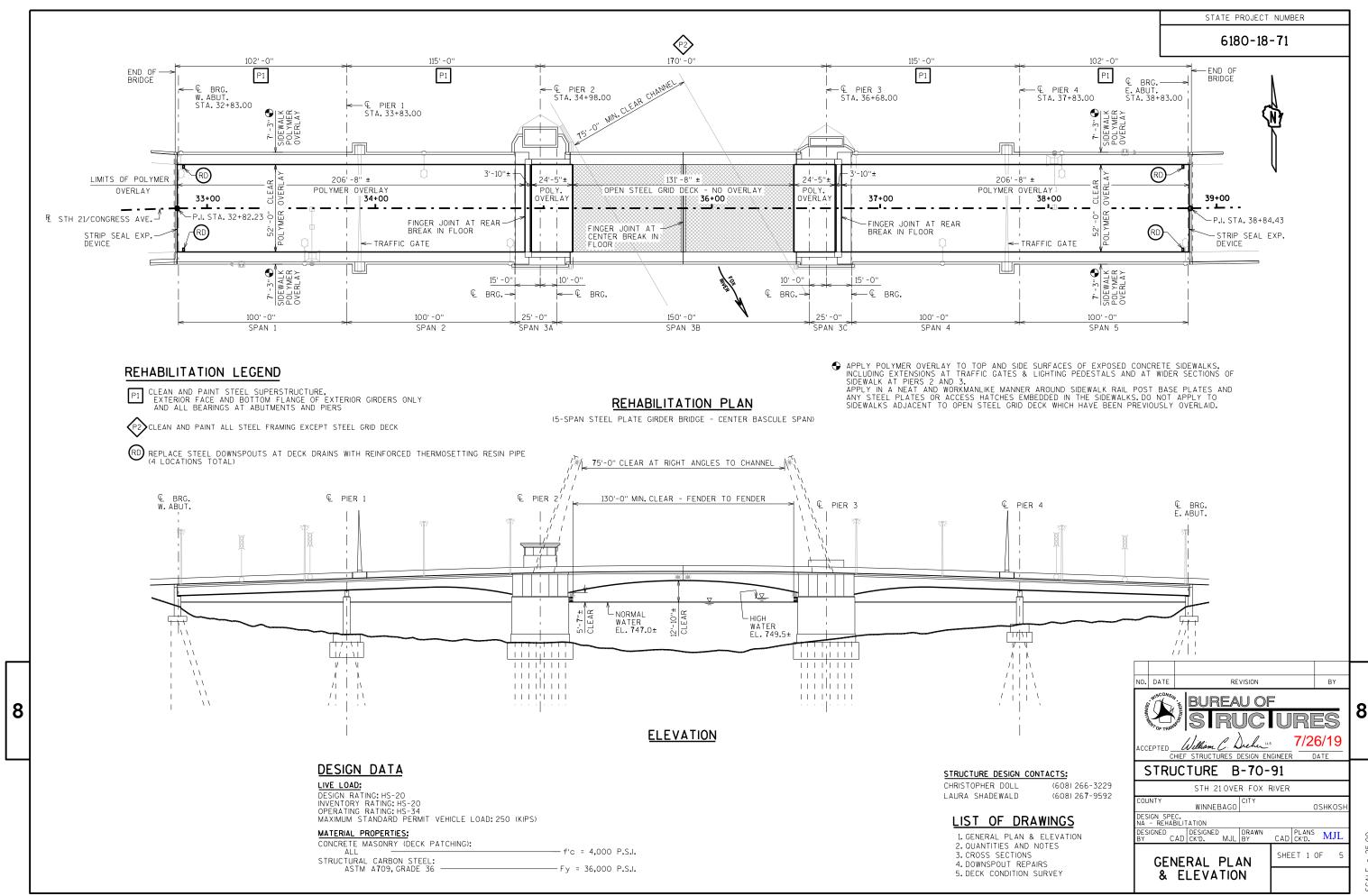
DATE 11/20/13 PLATE NO. <u>WO4-2.1</u>

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W042.DGN

PROJECT NO:

PLOT DATE: 20-NOV-2013 11:43



TOTAL ESTIMATED QUANTITIES

	BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS	
	509.0301	PREPARATION DECKS TYPE 1	SY	95	
	509.0302	PREPARATION DECKS TYPE 2	SY	1	
	509.0310.5	SAWING PAVEMENT DECK PREPARATION AREAS	LF	320	
	509.2000	FULL-DEPTH DECK REPAIR	SY	1	
	509.2100.S	CONCRETE MASONRY DECK REPAIR	CY	6	
	509.5100.5	POLYMER OVERLAY	SY	3,551	
	514.2625	DOWNSPOUT 6-INCH	LF	24	
	51 7. 1800.S	STRUCTURE REPAINTING RECYCLED ABRASIVE B-70-91	LS	1	*
☆	51 7. 3000.S	STRUCTURE OVERCOATING CLEANING AND PRIMING B-70-91	LS	1	*
☆	51 7. 4000.S	CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-70-91	LS	1	1
	51 7. 4500.S	NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-70-91	LS	1	
	51 7. 6001.S	PORTABLE DECONTAMINATION FACILITY	EACH	1	
	SPV.0105	BALANCE BASCULE BRIDGE LEAVES	LS	1	
					_

☆ BID ITEM INCLUDES CLEANING AND PAINTING INSIDE MACHINE ROOMS OF THE BASCULE SPAN ONLY.

ALL OTHER CLEANING AND PAINTING INCLUDED IN "STRUCTURE REPAINTING RECYCLED ABRASIVE B-70-91" AND

"NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-70-91".

* SEE "PAINTING NOTES" ON THIS SHEET FOR APPROXIMATE SQUARE FOOT QUANTITIES

8

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

THE COLOR OF THE FINISH EPOXY TOP COAT SHALL BE LIGHT BLUE, (AMS STANDARD 595A COLOR NO. 25240) OR SIMILAR COLOR APPROVED BY THE ENGINEER.

ALL FIELD CONNECTIONS SHALL BE MADE WITH A325 HIGH-TENSILE STRENGTH BOLTS UNLESS OTHERWISE SHOWN OR NOTED.

DECK & SIDEWALK SURFACE PROFILING/CLEANING IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

AREAS OF "PREPARATION DECKS TYPE 1" SHALL BE DEFINED BY A SAW CUT.

PREPARATION DECKS TYPE 1, PREPARATION DECKS TYPE 2, AND FULL-DEPTH DECK REPAIR AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE FIELD ENGINEER. DECK PREPARATION AND FULL-DEPTH DECK REPAIRS SHALL BE FILLED WITH "CONCRETE MASONRY DECK REPAIR".

SEE "DECK CONDITION SURVEY" SHEET FOR APPROXIMATE LOCATIONS OF DECK PREPARATION AREAS.

MASK/COVER ALL JOINT OPENINGS AND STEEL JOINT PLATES TO PREVENT OVERLAY MATERIAL FROM BEING APPLIED TO THE PLATES AND FILLING THE EXPANSION JOINTS.

PROVIDE TRANSITIONAL AREAS AT JOINT PLATES IN ACCORDANCE WITH THE "POLYMER OVERLAY" SPECIAL PROVISION.

BASCULE SPAN SHALL BE CHECKED FOR BALANCE AFTER THE PLACEMENT OF THE OVERLAY, SEE "BALANCE BASCULE BRIDGE LEAVES" SPECIAL PROVISION FOR REQUIREMENTS. THE MOVEABLE SPAN IS CURRENTLY IN A DESIREABLE STATE OF BALANCE, BEING SLIGHTLY THE PROPOSED OVERLAY MAY OR MAY NOT CREATE A SLIGHTLY COUNTERWEIGHT HEAVY STATE OF BALANCE, RESULTING IN A TENDENACY TOWARDS OPENING OF THE BASCULE LEAVES, WHICH IS NOT DESIREABLE.

PAINTING NOTES

BID ITEMS "STRUCTURE REPAINTING RECYCLED ABRASIVE B-70-91" AND "NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-70-91" PROVIDE FOR BLAST CLEANING AND PAINTING THE EXTERIOR FACES & BOTTOM FLANGES OF THE EXTERIOR GIRDERS IN SPANS 1-2 & 4-5, AND BLAST CLENAING AND PAINTING ALL EXPOSED AND ACCESSIBLE STRUCTURAL STEEL SURFACES IN SPAN 3 (BASCULE SPAN), EXCEPT FOR STEEL GRID DECK.

THE APPROXIMATE AREA OF "STRUCTURE REPAINTING RECYCLED ABRASIVE B-70-91" PAINTING IS AS FOLLOWS:

IS AS FOLLOWS: SPANS 1& 2 = 3,400 SF SPAN 3 = 28,500 SF SPAN 4 & 5 = 3,400 SF

DO NOT USE ABRASIVE BLASTING ON AREAS DIRECTLY ADJACENT TO OPEN GEARING, SEALS, BEARINGS, OR ANY OTHER MACHINERY WHERE GRIT MAY ENTER THE OPERATING SURFACES. FOR THESE AREAS, USE SSPC SP2 (HAND TOOL CLEANING).

BID ITEMS "STRUCTURE OVERCOATING CLEANING AND PRIMING B-70-91" AND "CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-70-91" PROVIDE FOR PAINTING INSIDE THE MACHINE ROOMS OF THE BASCULE SPAN. SPOT CLEANING AND PAINTING INSIDE THE MACHINE ROOMS SHALL BE PERFORMED WHERE DIRECTED BY THE ENGINEER.

THE APPROXIMATE AREA OF "STRUCTURE OVERCOATING CLEANING AND PRIMING B-70-91" PAINTING INSIDE THE MACHINE ROOMS IS 100 SF.

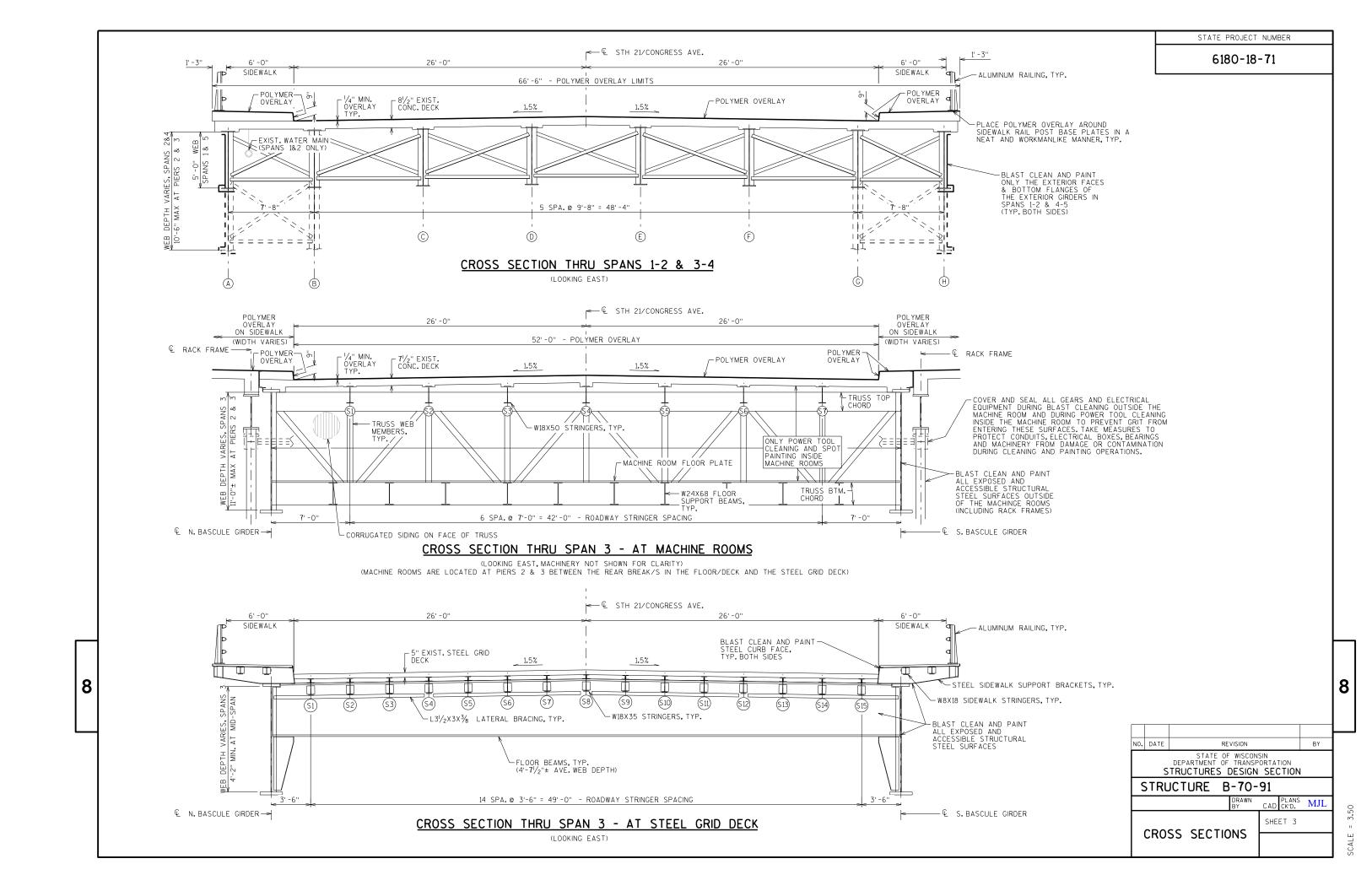
COVER AND SEAL ALL GEARS AND ELECTRICAL EQUIPMENT DURING POWER TOOL CLEANING INSIDE THE MACHINE ROOM TO PREVENT GRIT FROM ENTERING THESE SURFACES, TAKE MEASURES TO PROTECT CONDUITS, ELECTRICAL BOXES, BEARINGS AND MACHINERY FROM DAMAGE OR CONTAMINATION DURING CLEANING AND PAINTING OPERATIONS.

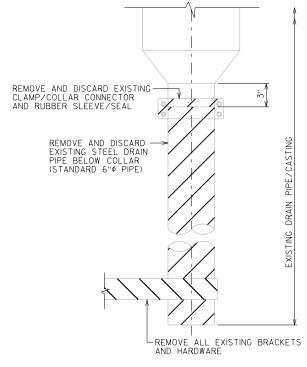
THE STEEL GRID DECK OF THE BASCULE SPAN ARE CONSTRUCTED OF GALVANIZED STEEL. THIS STEEL SHALL NOT BE BLAST CLEANED OR PAINTED. MEASURES SHALL BE TAKEN TO PROTECT THE GALVANIZING COATING DURING BLAST CLEANING AND PAINTING OF THE BASCULE GIRDERS, FLOOR BEAMS, AND STRINGERS.

COLD WEATHER PAINTING DURING WINTER MONTHS IS ANTICIPATED FOR THE MAJORITY OF THE PAINTING WORK (PRIOR TO THE BEGINNING OF BOATING SEASON IN THE SPRING). ALL COST ASSOCIATED WITH PROVIDING HEATED ENCLOSURES TO ALLOW PAINTING DURING FREEZING TEMPERATURES SHALL BE INCLUDED IN THE RESPECTIVE PAINTING BID ITEMS.

NO.	DATE		DE	VISION			BY					
NO.	DATE		IVL	VISION			יט					
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION												
0	STRUCTURE B-70-91											
				DRAWN BY	CAD	PLANS CK'D.	MJL					
	QUA	NTITIES	SHE	ET 2								
		NOTES										

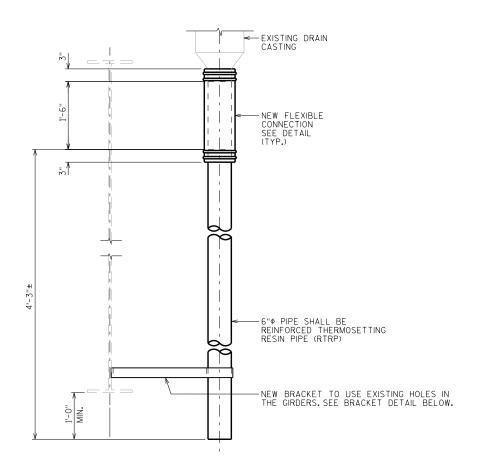
8



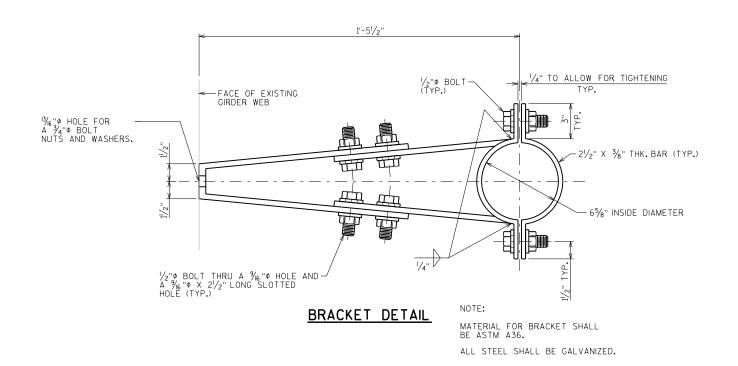


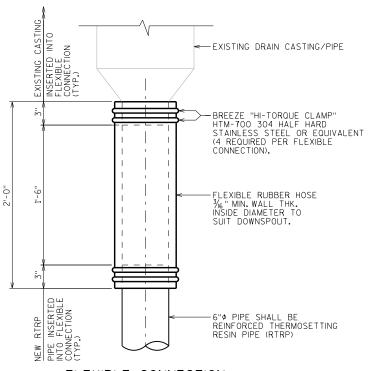
EXISTING DRAIN PIPE

SHOWING REMOVAL
REMOVAL WORK TO BE INCLUDED UNDER BID
ITEM "DOWNSPOUT 6-INCH"



ELEVATION





FLEXIBLE CONNECTION

1 PER FLOOR DRAIN (4 TOTAL)
INCLUDED UNDER BID ITEM "DOWNSPOUT 6-INCH"

NOTES:

ALL WORK AND MATERIALS SHOWN ON THIS SHEET SHALL BE INCLUDED IN BID ITEM "DOWNSPOUT 6-INCH".

NO. DATE REVISION BY

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION

STRUCTURE B-70-91

DRAWN CAD CK'D. MJL

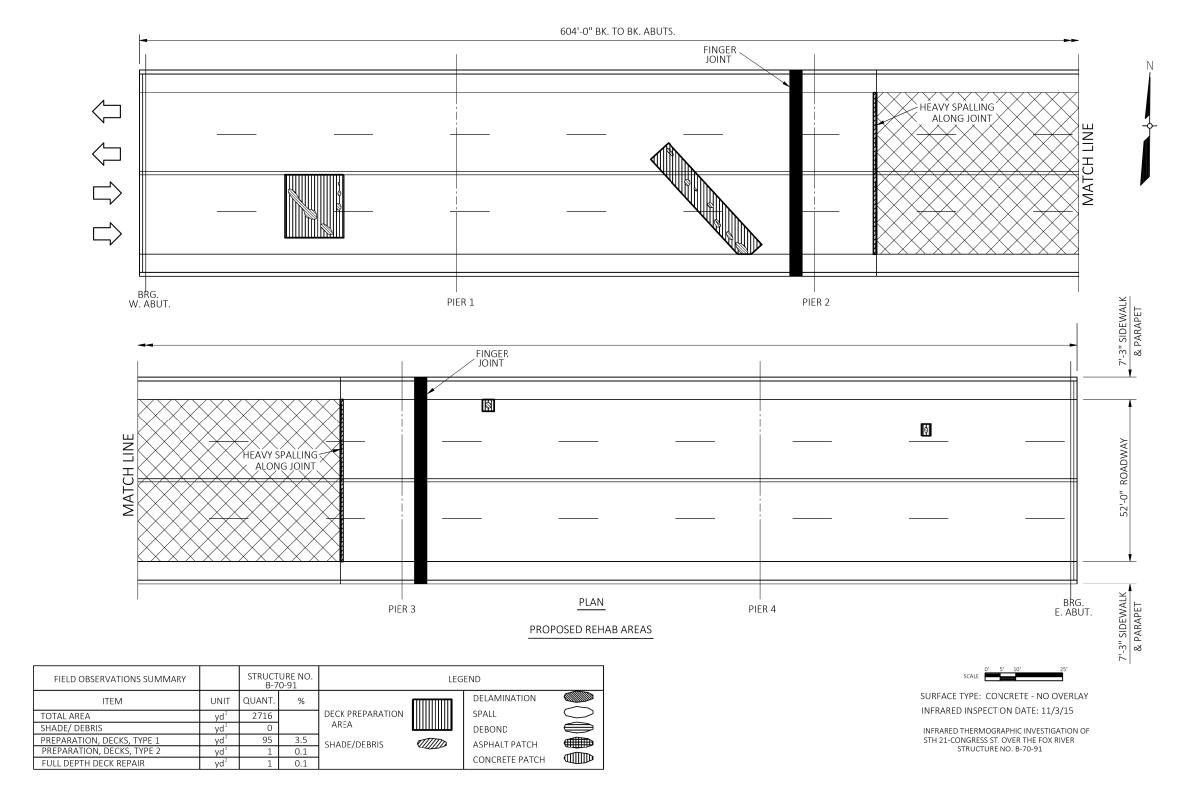
DOWNSPOUT REPAIRS

8

SCALE - 100

STATE PROJECT NUMBER

6180-18-71



8

<u>NOTES</u>

DECK SURVEY IS SHOWN FOR INFORMATION PURPOSES ONLY AND MAY NOT BE ALL INCLUSIVE. ADDITIONAL PATCHING/PREPARATION MAY BE REQUIRED AND SHALL BE PERFORMED AS DIRECTED BY THE ENGINEER.

NO.		BY										
	I											
5												
			DRAWN BY CA	PLANS D CK'D.	MJL							

SHEET 5

DECK CONDITION SURVEY

8



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

http://www.dot.wisconsin.gov