

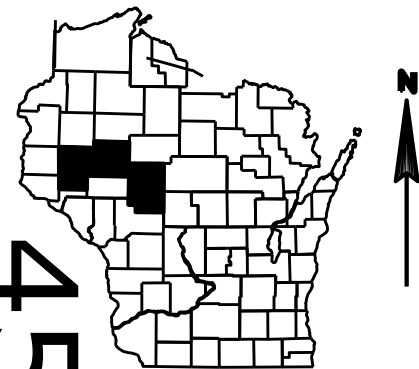
EAU
PROJECT ID: 1050-11-80
WITH: 1050-11-81
COUNTY: CHIPPEWA, DUNN, CLARK

MAY 2019

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plot
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 24



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STATE HIGHWAY REHABILITATION - MAINTENANCE PROJECT

NW REGION, VAR CTYS
STATE HIGHWAY 29

BRIDGE DECK SEALING SFY 2019 (VAR)

STH 029
CHIPPEWA COUNTY

STATE PROJECT NUMBER
1050-11-80

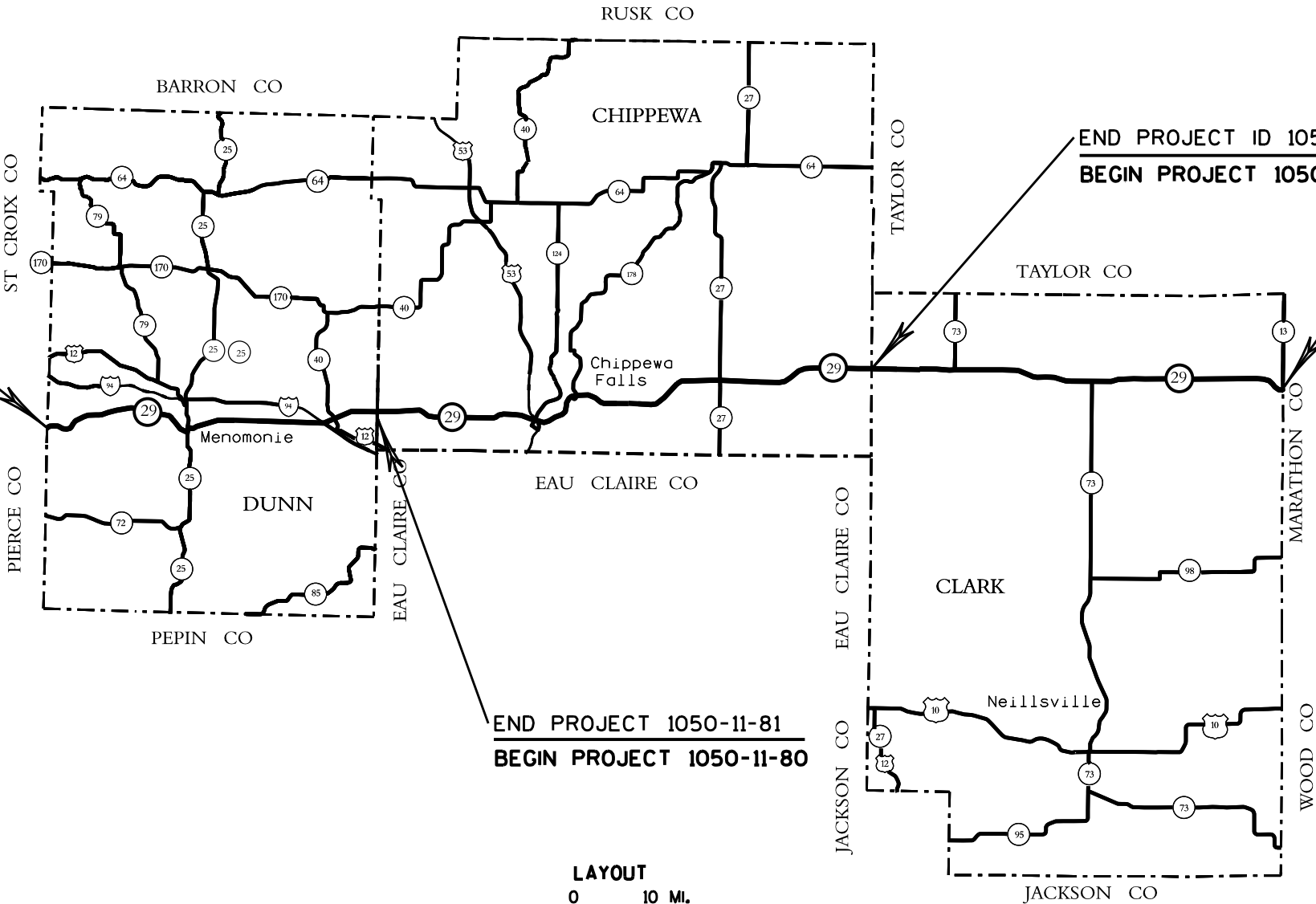
NW REGION, VAR CTYS
STATE HIGHWAY 29

BRIDGE DECK SEALING SFY 2019 (VAR)

STH 029
CLARK COUNTY

STATE PROJECT NUMBER
1050-11-81

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1050-11-80	WISC 2019320	1
1050-11-81	WISC 2019321	1



DESIGN DESIGNATION

A.A.D.T.	= N/A
A.A.D.T.	= N/A
D.H.V.	= N/A
D.D.	= N/A
T.	= N/A
DESIGN SPEED	= N/A
ESALS	= N/A

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	////
REFERENCE LINE	—
COMBUSTIBLE FLUIDS	CAUTION
UTILITIES	
ELECTRIC	— E —
FIBER OPTIC	— FO —
GAS	— G —
SANITARY SEWER	— SAN —
STORM SEWER	— SS —
TELEPHONE	— T —
WATER	— W —
UTILITY PEDESTAL	⊕
POWER POLE	⊗

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	N/A
Designer	DOUGLAS ANDERSON
Project Manager	DAVID KOEPP, P.E.
District Examiner	JENNIFER OLDENBERG
District Supervisor	JAMES KOENIG, P.E.
C.O. Examiner	

APPROVED FOR DISTRICT OFFICE

DATE: _____ (Signature)

GENERAL NOTES

- 1) WORK CONSISTS OF PREPARATION OF UPPER SIDE OF CONCRETE DECK, PARAPET SURFACE AND PIER COLUMNS AND APPLICATION OF CONCRETE PROTECTIVE SURFACE TREATMENT IN ACCORDANCE WITH MANUFACTURE'S SPECIFICATIONS AND RECOMMENDATIONS.
- 2) THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR WILL COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.
- 3) STH 29 WILL BE KEPT OPEN TO A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION AT ALL TIMES.
- 4) LANE CLOSURES WILL ONLY BE ALLOWED DURING TIMES SPECIFIED IN THE SPECIAL PROVISIONS.
- 5) SIGNS AND DEVICES WILL BE IN CONFORMANCE WITH THE LATEST WIMANUAL OF TRAFFIC CONTROL DEVICES (WMUTCD).
- 6) THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES WILL BE ADJUSTED TO FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

RAILROAD NOTES

RAILROAD INSURANCE AND COORDINATION REQUIRED FOR ANY AND ALL WORK ON STRUCTURES THAT SPAN PROPERTY OWNED BY RAILROAD COMPANIES. WORK UNDER CONTRACT INVOLVES WORK ON THE UPPER SIDE OF STRUCTURES THAT SPAN PROPERTY OWNED BY THE FOLLOWING RAILROAD COMPANIES. SEE SPECIAL PROVISIONS FOR CONTACT INFORMATION.

- UNION PACIFIC RAILROAD COMPANY
- WISCONSIN CENTRAL LTD.

**AREA CONTACTS****WISDOT NW REGION CONTACTS**

DAVID KEOPP
718 W. CLAIREMONT AVENUE
EAU CLAIRE, W 54701
(715) 836-2078

WIDNR (CHIPPEWA, DUNN, CLARK)

CHRIS WILLGER
DNR WEST CENTRAL REGION HO
1300 WEST CLAIRMONT STREET
EAU CLAIRE, WI 54701
(715) 836-1609

WISDOT NW REGION COUNTY CONTACTS**DUNN COUNTY**

MIKE BURKE
(715) 577-5395

CHIPPEWA COUNTY

RICHARD RICKSECKER
(715) 225-9318

CLARK COUNTY

ROSS KRAUSE
(715) 577-9544

NW REGION MAINTENANCE SUPERVISOR

JEFF KERN
CELL: (715) 836-2919

DECK NOTES

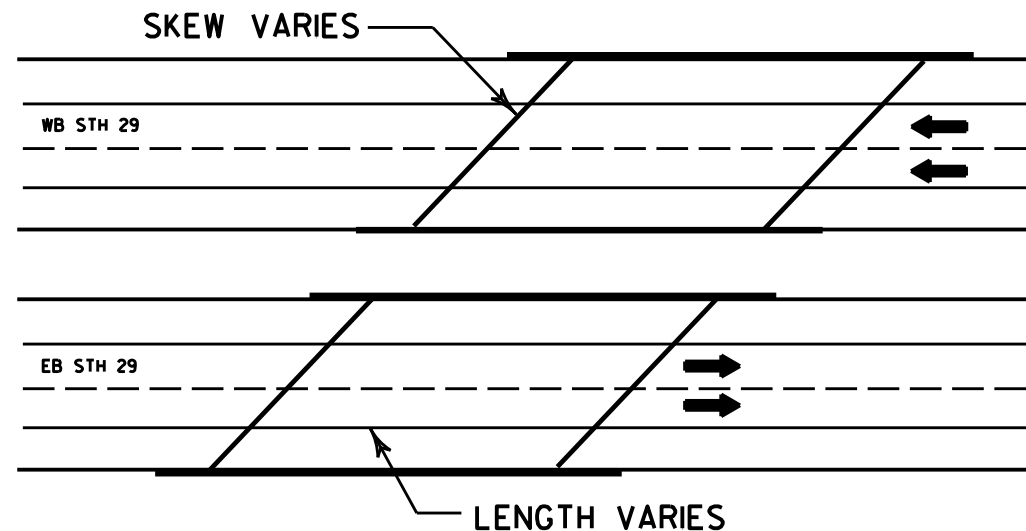
- ENTIRE DECK SURFACE TO BE CLEANED AND SEALED.
- INLET PROTECTION REQUIRED DURING CLEANING AND SEALING WORK ON THE STRUCTURES WITH FLOOR DRAINS.
- USE TYPE FF FABRIC DURING CLEANING ACTIVITIES.
- USE IMPERVIOUS FABRIC DURING SEALING ACTIVITIES.

PIER COLUMN NOTES

- PIER COLUMN SEALING ESTIMATED QUANTITY IS BASED ON A 10-FT COLUMN HEIGHT FOR EACH COLUMN ON EACH PIER.

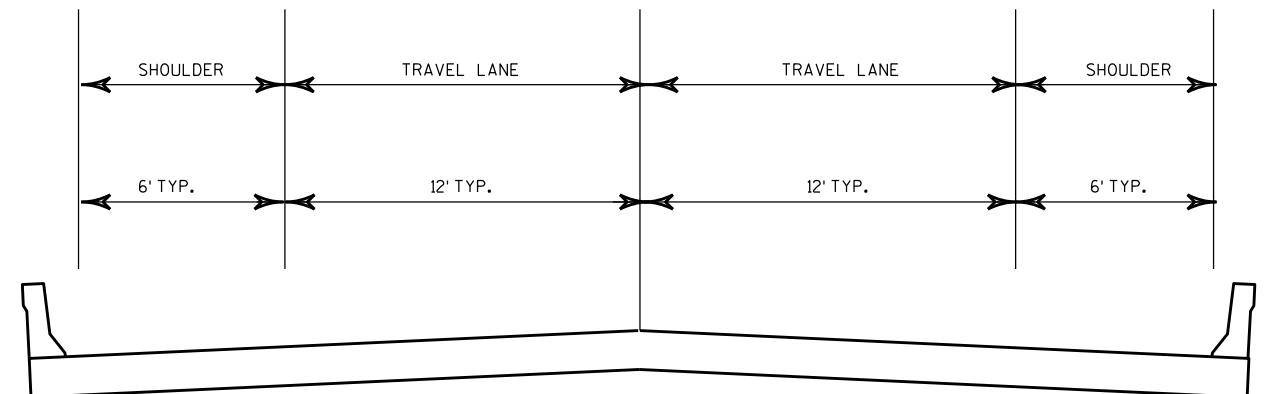
PARAPET NOTES

- INSIDE FACE AND TOP TO BE CLEANED AND SEALED.
- TOP SURFACE TO BE ROLLED OR BRUSHED.
- PARAPET TYPE AND HEIGHT VARIES.
- SURFACE AREA INCLUDED IN QUANTITY SUMMARY.
- AVERAGE 2'-6" HEIGHT USED FOR QUANTITY ESTIMATE.

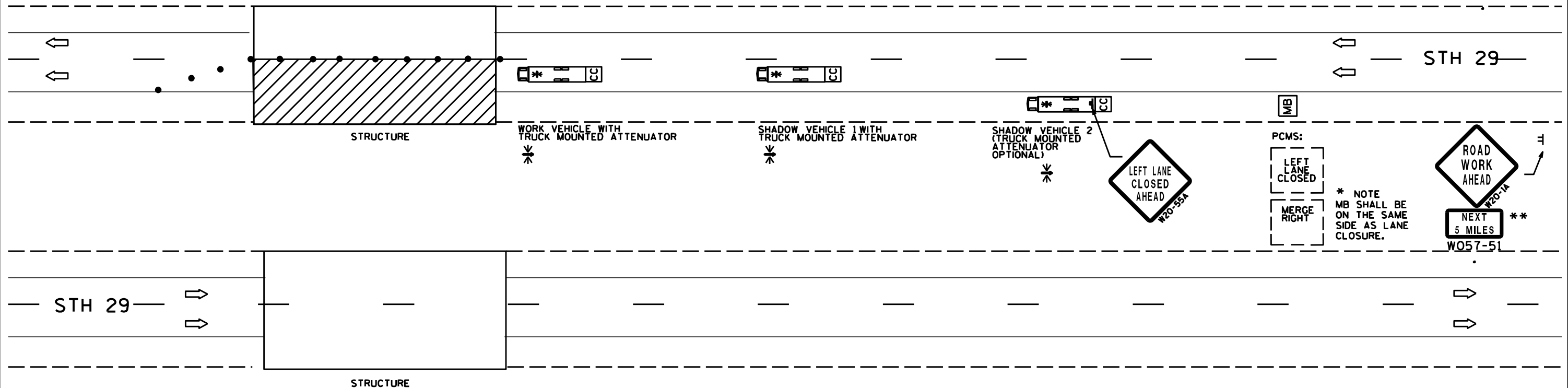
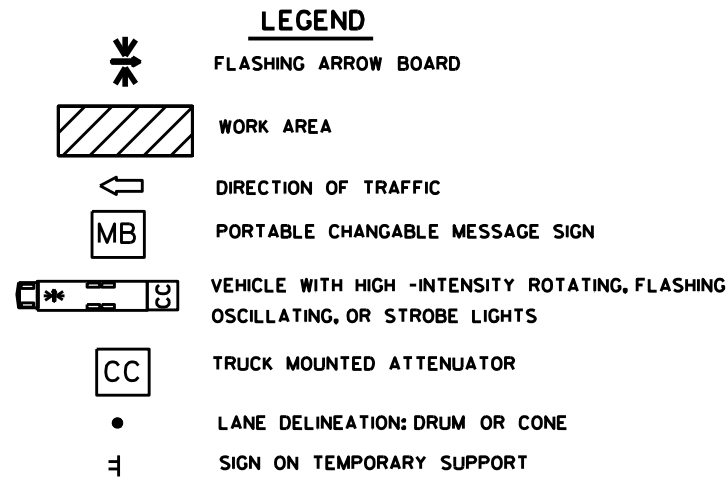


TYPICAL DECK PLAN VIEW
4 LANE DIVIDED HWY - TYPICAL STH 29

TYP. 10' SHLDR
TYP. 12' LANE
TYP. 12' LANE
SHLDR VARIES (6' TYP.)
TYP. 60' MEDIAN
SHLDR VARIES (6' TYP.)
TYP. 12' LANE
TYP. 12' LANE
TYP. 10' SHLDR



TYPICAL DECK CROSS SECTION VIEW
4 LANE DIVIDED HWY - TYPICAL STH 29



STANDARD NOTES:

ARROW BOARDS SHALL, AS A MINIMUM BE TYPE B, WITH A SIZE OF 60 X 30 INCHES

VEHICLE MOUNTED SIGNS SHALL BE MOUNTED IN A MANNER SUCH THAT THEY ARE NOT OBSTRUCTED BY EQUIPMENT OR SUPPLIES. SIGN LEGENDS ON VEHICLE-MOUNTED SIGNS SHALL BE COVERED OR TURNED FROM TRAFFIC WHEN WORK IS NOT IN PROGRESS.

SHADOW AND WORK VEHICLES SHALL DISPLAY HIGH-INTENSITY ROTATING, FLASHING, OSCILLATING, OR STROBE LIGHTS.

AN ARROW BOARD SHALL BE USED WHEN A FREEWAY LANE IS CLOSED. WHEN MORE THAN ONE FREEWAY LANE IS CLOSED, A SEPARATE ARROW BOARD SHALL BE USED FOR EACH LANE OF CLOSURE.

ALL SIGNS SHALL BE 48 INCHES x 48 INCHES, UNLESS OTHERWISE NOTED.

GENERAL NOTES:

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING LEFT LANE - REVERSE FOR CLOSING RIGHT LANE.

MULTIPLE PCMS MAY BE NECESSARY BASED UPON ON ROADWAY GEOMETRICS AND LOCATIONS OF ENTRANCE/EXIT RAMPs OR SIDE ROADS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

THESE GENERAL NOTES ARE APPLICABLE TO STH 29 FOR FREEWAY, SHORT TERM STATIONARY, OFF-PEAK TIME OPERATIONS. FOLLOW SDD 15D12 IF LANE CLOSURES IS IN PLACE LONG-TERM.

** 5 MILE MINIMUM. RELOCATE SIGNS AS WORK PROGRESSES AND NECESSARY OR AS DIRECTED BY THE ENGINEER.

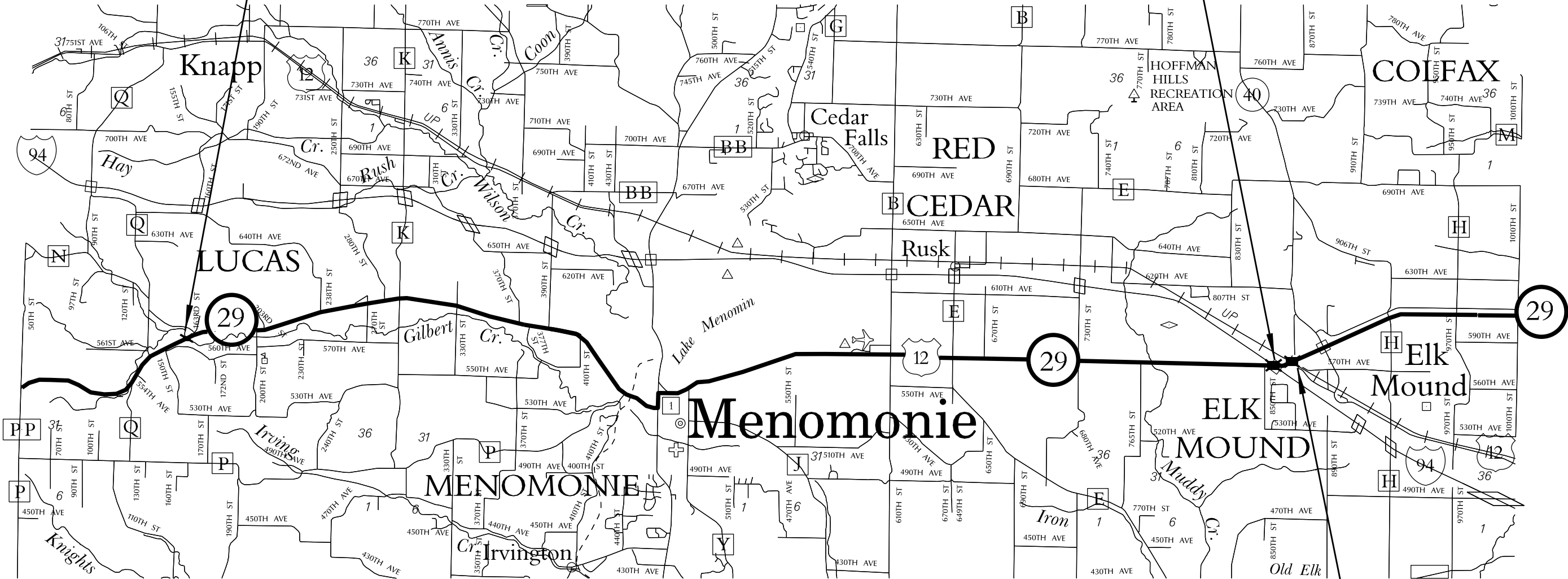
PROJECT 1050-11-81: DUNN COUNTY STRUCTURE SUMMARY

STRUCTURE ID	SURFACE AREA (SF)	HWY ON	FEATURE UNDER	TYPE	COUNTY	MUNICIPALITY
B-17-0059	1,695.60	IH-94 WB	USH12 STH29	BRIDGE COLUMN	DUNN	TOWN OF ELK MOUND
B-17-0050	1,695.60	IH-94 EB	USH12 STH29	BRIDGE COLUMN	DUNN	TOWN OF ELK MOUND
B-17-0075	4,440.00	STH 029	GILBERT CREEK	BRIDGE DECK	DUNN	TOWN OF LUCAS
B-17-0116	8,207.00	USH12 STH29	CNW RR	BRIDGE DECK	DUNN	TOWN OF ELK MOUND
B-17-0163	7,308.00	USH12 STH29	UNION PACIFIC RR	BRIDGE DECK	DUNN	TOWN OF ELK MOUND



STRUCTURE B-17-0075
ON: STH 029
UNDER: GILBRT CREEK

STRUCTURE B-17-0059
ON: IH-94 WB
SRUCTURE B-17-0060
ON: IH-94 EB
UNDER: USH12 STH29



STRUCTURE B-17-0116
ON: USH12 STH29 WB
STRUCTURE: B-17-0163
ON: USH12 STH29 EB
UNDER: UNION PACIFIC RR



STRUCTURE B-09-0213
ON: STH 29 WB
STRUCTURE B-09-0214
ON: STH 29 EB
UNDER: UP RAILROAD

STRUCTURE B-09-0209
ON: STH 29 WB
STRUCTURE B-09-0210
ON: STH 29 EB
UNDER: CHIPPEWA RIVER

STRUCTURE B-09-0243
ON: STH 29 WB
STRUCTURE B-09-0244
ON: STH 29 EB
UNDER: 103RD STREET

STRUCTURE B-09-0206
ON: STH 29 WB
STRUCTURE B-09-0207
ON: STH 29 EB
UNDER: ELK CREEK

STRUCTURE B-09-0220
ON: CTH P
UNDER: STH 29

STRUCTURE B-09-0217
ON: STH 29 WB
STRUCTURE B-09-0218
ON: STH 29 EB
UNDER: USH 53

STRUCTURE B-09-0215
ON: STH 29 WB
STRUCTURE B-09-0216
ON: STH 29 EB
UNDER: STH 124

STRUCTURE B-09-0221
ON: 40TH AVE
UNDER: STH 29

STRUCTURE B-09-0030
ON: CTH X / 37TH AVE
UNDER: STH 29

STRUCTURE B-09-0032
ON: 160TH STREET
UNDER: STH 29

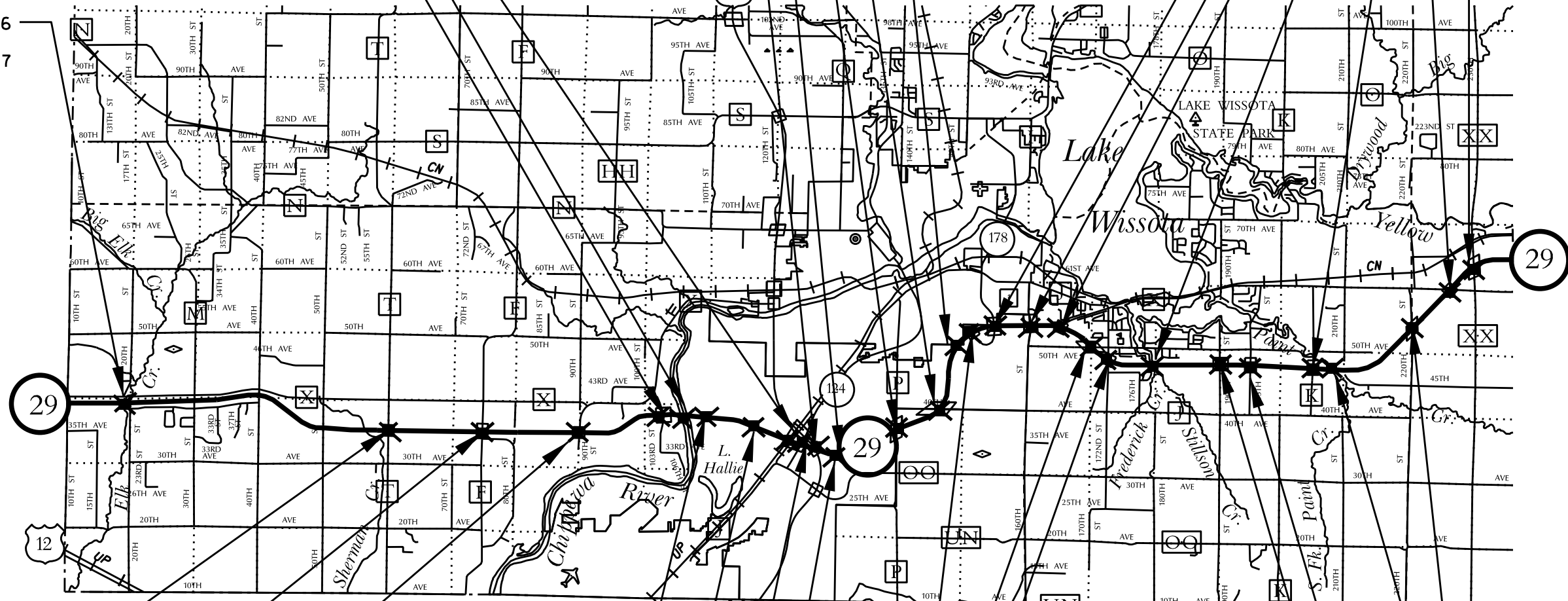
STRUCTURE B-09-0225
ON: SEY MOUR CRAY BLVD
STRUCTURE B-09-0226
ON: SEY MOUR CRAY BLVD
UNDER: STH 29

STRUCTURE B-09-0222
ON: 50TH AVE
UNDER: STH 29

STRUCTURE B-09-0031
ON: STH 29 WB
STRUCTURE B-09-0171
ON: STH 29 EB
UNDER: STILLSON CREEK
STRUCTURE B-09-0037
ON: CTH K / STILLSON RD
UNDER: STH 29

STRUCTURE B-09-0019
ON: CTH X
UNDER: STH 29

STRUCTURE B-09-0176
ON: CTH XX
UNDER: STH 29



STRUCTURE B-09-0208
ON: CTH T
UNDER: STH 29

STRUCTURE B-09-0241
ON: CTH F
UNDER: STH 29

STRUCTURE B-09-0242
ON: 90TH STREET
UNDER: STH 29

STRUCTURE B-09-0211
ON: 110TH STREET
UNDER: STH 29

STRUCTURE B-09-0212
ON: 120TH STREET
UNDER: STH 29

STRUCTURE B-09-0245
ON: STH 29 OFF RAMP
UNDER: STH 124

STRUCTURE B-09-0034
ON: CTH J
UNDER: STH 29

STRUCTURE B-09-0033
ON: 50TH AVE
UNDER: STH 29

STRUCTURE B-09-0223
ON: SEY MOUR CRAY BLVD
UNDER: STH 29

STRUCTURE B-09-0219
ON: 130TH STREET
UNDER: STH 29

STRUCTURE B-09-0035
ON: STH 29 WB
STRUCTURE B-09-0174
ON: STH 29 EB
UNDER: 190TH STREET

STRUCTURE B-09-0038
ON: 220TH STREET
UNDER: STH 29

STRUCTURE B-09-0038
ON: STH 29 WB
STRUCTURE B-09-0175
ON: STH 29 EB
UNDER: PAINT CREEK

STRUCTURE B-09-0036
ON: 195TH STREET
UNDER: STH 29



STRUCTURE B-09-0177
ON: STH 29 WB
STRUCTURE B-09-0020
ON: STH 29 EB
UNDER: CTH X

STRUCTURE B-09-0190
ON: STH 29 WB
STRUCTURE B-09-0191
ON: STH 29 EB
UNDER: CTH H

STRUCTURE B-09-0022
ON: CTH X
UNDER: STH 29

STRUCTURE B-17-0020
ON: HALL ESTATE RD
UNDER: IH-94

STRUCTURE B-17-0020
ON: HALL ESTATE RD
UNDER: IH-94

PROJECT 1050-11-80: CHIPPEWA COUNTY STRUCTURE SUMMARY

STRUCT.ID	SURFACE (SF)	HWY ON	FEATURE UNDER	TYPE	COUNTY	MUNCIPALITY
B-09-0019	1,130.40	x	STH 29	BRIDGE COLUMNS	CHIP.	TSIGEL
B-09-0020	7,365.00	SHT 29 EB	x	BRIDGE DECK	CHIP.	T-SIGEL
B-09-0022	1,130.40	X 37TH AVE	STH 29	BRIDGE COLUMNS	CHIP.	V-BOYD
B-09-0030	847.80	STH 29 WB	STH 29	BRIDGE COLUMNS	CHIP.	T-LAFAYETTE
B-09-0031	5,142.00	160TH ST.	STILLSON CREK	BRIDGE DECK	CHIP.	T-LAFAYETTE
B-09-0032	847.80	50TH AVE	STH 29	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0033	847.80	J	STH 29	BRIDGE COLUMNS	CHIP.	T-LAFAYETTE
B-09-0034	847.80	STH 29 WB	STH 29	BRIDGE COLUMNS	CHIP.	T-LAFAYETTE
B-09-0035	4,454.00	195TH ST.	190TH ST	BRIDGE DECK	CHIP.	T-LAFAYETTE
B-09-0036	847.80	K STILLSON RD.	STH 29	BRIDGE COLUMNS	CHIP.	T-LAFAYETTE
B-09-0037	847.80	STH 29 WB	STH 29	BRIDGE COLUMNS	CHIP.	T-LAFAYETTE
B-09-0038	4,379.00	220TH ST.	PAINT CREEK	BRIDGE DECK	CHIP.	T-LAFAYETTE
B-09-0039	847.80	STH 29EB	STH 29	BRIDGE COLUMNS	CHIP.	T-LAFAYETTE
B-09-0171	6,991.00	STH 29 EB	STILLSON CREEK	BRIDGE DECK	CHIP.	T-LAFAYETTE
B-09-0174	4,205.00	STH 29 EB	190TH ST.	BRIDGE DECK	CHIP.	T-LAFAYETTE
B-09-0175	4,596.00	XX	PAINT CREEK	BRIDGE DECK	CHIP.	T-LAFAYETTE
B-09-0176	282.60	STH 29 WB	STH 29	BRIDGE COLUMNS	CHIP.	T-SIGEL
B-09-0177	7,314.00	STH 27	X	BRIDGE DECK	CHIP.	T-SIGEL
B-09-0179	1,130.40	D	STH 29	BRIDGE COLUMNS	CHIP.	V-CADOTT
B-09-0189	1,507.20	STH 29 WB	STH 29	BRIDGE COLUMNS	CHIP.	V-BOYD
B-09-0190	6,153.00	STH 29 EB	H	BRIDGE DECK	CHIP.	T-DELMAR
B-09-191	6,153.00	STH 29 WB	H	BRIDGE DECK	CHIP.	T-DELMAR
B-09-0206	3,219.00	STH 29 EB	ELK CREEK	BRIDGE DECK	CHIP.	T-WHEATON
B-09-0207	3,591.00	CTH T	ELK CREEK	BRIDGE DECK	CHIP.	T-WHEATON
B-09-0208	565.20	STH 29	STH 29	BRIDGE COLUMNS	CHIP.	T-WHEATON
B-09-0209	36,062.00	STH 29	CHIPPEWA RIVER	BRIDGE DECK	CHIP.	T-WHEATON
B-09-0210	29,757.00	CTH T	CHIPPEWA RIVER	BRIDGE DECK	CHIP.	T-WHEATON
B-09-0211	282.60	STH 29	STH 29 WB	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0212	565.20	110TH ST.	STH 29 WB	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0213	10,553.00	120TH ST.	UP RAILROAD	BRIDGE DECK	CHIP.	T-HALLIE
B-09-0214	8,368.00	STH 29 WB	UP RAILROAD	BRIDGE DECK	CHIP.	T-HALLIE
B-09-0215	11,450.00	STH 29 EB	STH 124	BRIDGE DECK	CHIP.	T-HALLIE
B-09-0216	8,929.00	STH 29 WB	STH 124	BRIDGE DECK	CHIP.	T-HALLIE
B-09-0217	21,143.00	STH 29 EB	USH 53 NB	BRIDGE DECK	CHIP.	T-HALLIE
B-09-0218	19,420.00	STH 29 WB	USH 53	BRIDGE DECK	CHIP.	T-HALLIE
B-09-0219	282.60	STH 29 EB	STH 29	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0220	282.60	130TH ST.	STH 29	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0221	282.60	CTH P	STH 29	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0222	282.60	40TH AVE	STH 29	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0223	565.20	50TH AVE	STH 29	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0225	282.60	SEYMOUR CRAY BLVD	STH 29	BRIDGE COLUMNS	CHIP.	T-WHEATON
B-09-0226	282.60	SEYMOUR CRAY BLVD	STH 29 WB	BRIDGE COLUMNS	CHIP.	T-HALLIE
B-09-0241	282.60	CTH F	STH 29 WB	BRIDGE COLUMNS	CHIP.	T-WHEATON
B-09-0242	376.80	90TH ST.	STH 29	BRIDGE COLUMNS	CHIP.	T-WHEATON
B-09-0243	4,380.00	STH 29	103RD ST.	BRIDGE DECK	CHIP.	T-WHEATON
B-09-0244	4,380.00	STH 29	103RD ST.	BRIDGE DECK	CHIP.	T-WHEATON
B-09-0245	5,921.00	STH 29 OFF RAMP	STH 124	BRIDGE DECK	CHIP.	T-HALLIE

PROJECT 1050-11-81: CLARK COUNTY STRUCTURE SUMMARY

SRUCTURE ID	SURFACE (SF)	HWY ON	FEATURE UNDER	TYPE	COUNTY	MUNCIPALITY
B-10-0017	2,562.00	STH 29	DILL CREEK	BRIDGE DECK	CLARK	T-COLBY
B-10-0019	11,340.00	STH 29/STH73	BLACK RIVER	BRIDGE DECK	CLARK	T-LONGWOOD
B-10-0020	7,589.00	STH 29	POPPLE RIVER	BRIDGE DECK	CLARK	C-OWEN
B-10-0022	5,694.00	STH 29	CTH D	BRIDGE DECK	CLARK	T-LONGWOOD
B-10-0023	7,293.00	STH 29	WC RAILROAD	BRIDGE DECK	CLARK	T-GREEN GROVE
B-10-0143	5,826.00	STH 29 WB	N FK EAU CLAIRE RVR	BRIDGE DECK	CLARK	T-THORP
B-10-0144	6,097.00	STH 29 WB	STH 73 NB	BRIDGE DECK	CLARK	T-WITHEE
B-10-0145	6,097.00	STH 29 EB	STH 73 NB	BRIDGE DECK	CLARK	T-WITHEE
B-10-0146	5,826.00	STH 29 EB	N FK EAU CLAIRE RVR	BRIDGE DECK	CLARK	T-THORP
B-10-0149	5,078.00	STH 29 EB	N FK POPPLE RIVER	BRIDGE DECK	CLARK	T-GREEN GROVE
B-10-0151	282.60	HILINE AVE	STH 29 WB	BRIDGE COLUMNS	CLARK	T-COLBY
B-10-0168	4,450.00	STH29WB/STH73	S FK EAU CLAIRE RVR	BRIDGE DECK	CLARK	T-WITHEE
B-10-0169	4,450.00	STH29EB/STH73	S FK EAU CLAIRE RVR	BRIDGE DECK	CLARK	T-WITHEE
B-10-0170	13,966.00	STH29-STH73	BLACK RIVER	BRIDGE DECK	CLARK	T-LONGWOOD
B-10-0171	8,372.00	STH 29	POPPLE RIVER	BRIDGE DECK	CLARK	C-OWEN
B-10-0173	7,279.00	STH 29	CTH D	BRIDGE DECK	CLARK	T-GREEN GROVE
B-10-0174	8,251.00	STH 29 WB	WC LTD RAILROAD	BRIDGE DECK	CLARK	T-GREEN GROVE
B-10-0175	5,078.00	STH 29 WB	N FK POPPLE RIVER	BRIDGE DECK	CLARK	T-GREEN GROVE
B-10-0176	2,890.00	STH 29	DILL CREEK	BRIDGE DECK	CLARK	T-COLBY
B-10-0177	1,130.40	STH 73 NB	STH 29	BRIDGE COLUMNS	CLARK	T-LONGWOOD
B-10-0178	376.80	CARDINAL AVE	STH 29	BRIDGE COLUMNS	CLARK	T-GREEN GROVE
B-10-0179	753.60	CTH E	STH 29	BRIDGE COLUMNS	CLARK	V-CURTISS



STRUCTURE B-10-0179
ON: CTH E
UNDER: STH 29

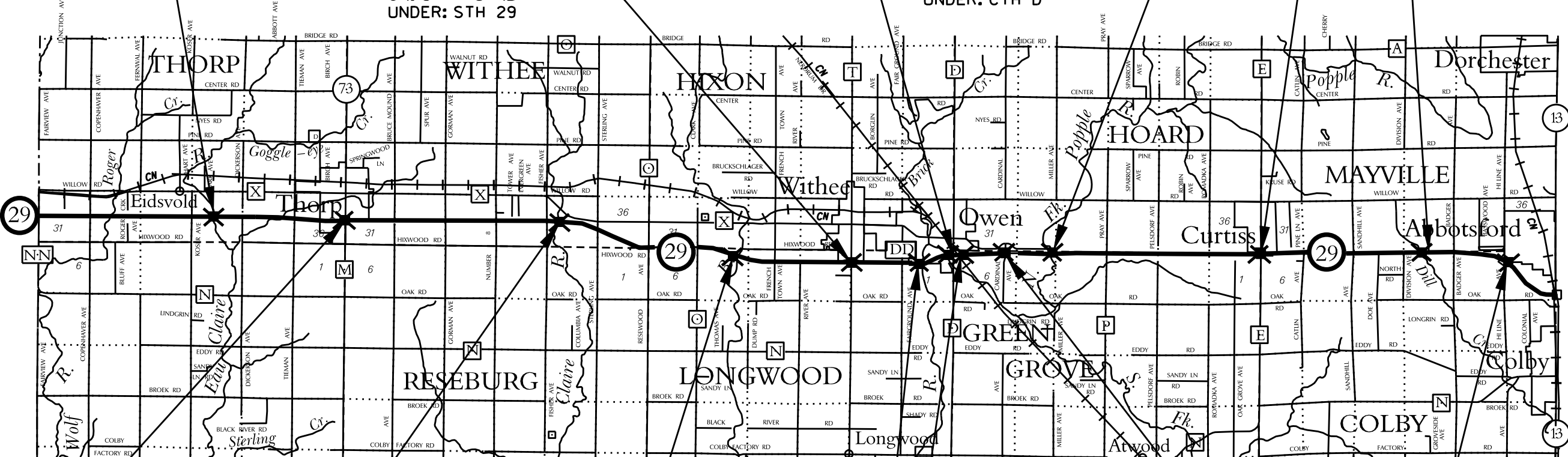
STRUCTURE B-10-0176
ON: STH 29 WB
STRUCTURE B-10-0017
ON: STH 29 EB
UNDER: DILL CREEK

STRUCTURE B-10-0143
ON: STH 29 WB
STRUCTURE B-10-0146
ON: STH 29 EB
UNDER: N FK EAU CLAIRE RVR

STRUCTURE B-10-0177
ON: STH 73 NB
UNDER: STH 29

STRUCTURE B-10-0017
ON: STH 29
STRUCTURE B-10-0019
ON: STH 29
UNDER: DILL CK/BLK RVR

STRUCTURE B-10-0173
ON: STH 29 WB
STRUCTURE B-10-0022
ON: STH 29 EB
UNDER: CTH D



STRUCTURE B-10-0144
ON: STH 29 WB
STRUCTURE B-10-0145
ON: STH 29 EB
UNDER: STH 73 NB

STRUCTURE B-10-0168
ON: STH29WB/STH73NB
STRUCTURE B-10-0169
ON: STH29EB/STH73SB
UNDER: S FK EAU CLAIRE RVR

STRUCTURE B-10-0170
ON: STH29WB/STH73
STRUCTURE B-10-00199
ON: STH29EB/STH73
UNDER: BLACK RIVER

STRUCTURE B-10-0171
ON: STH 29 WB
STRUCTURE B-10-0020
ON: STH 29 EB
UNDER: POPPLE RIVER

STRUCTURE B-10-0174
ON: STH29WB
STRUCTURE B-10-0023
ON: STH29EB
UNDER: WC LTD RR

STRUCTURE B-10-0178
ON: CARDINAL AVE
UNDER: STH 29

STRUCTURE B-10-0151
ON: HILINE AVE
UNDER: STH29WB

Estimate Of Quantities

				1050-11-80	1050-11-81	
Line	Item	Item Description	Unit	Total	Qty	Qty
0002	213.0100	Finishing Roadway (project) 01. 1050-11-80	EACH	1.000	1.000	
0004	213.0100	Finishing Roadway (project) 01. 1050-11-81	EACH	1.000		1.000
0006	502.3215.S	Protective Surface Treatment Reseal	SY	42,106.900	28,698.100	13,408.800
0008	619.1000	Mobilization	EACH	1.000	0.620	0.380
0010	628.7010	Inlet Protection Type B	EACH	12.000		12.000
0012	643.0300	Traffic Control Drums	DAY	5,500.000	3,300.000	2,200.000
0014	643.0420	Traffic Control Barricades Type III	DAY	120.000	70.000	50.000
0016	643.0705	Traffic Control Warning Lights Type A	DAY	225.000	135.000	90.000
0018	643.0715	Traffic Control Warning Lights Type C	DAY	1,630.000	980.000	650.000
0020	643.0800	Traffic Control Arrow Boards	DAY	225.000	135.000	90.000
0022	643.0900	Traffic Control Signs	DAY	1,630.000	980.000	650.000
0024	643.1051	Traffic Control Signs PCMS with Cellular Communications	DAY	50.000	25.000	25.000
0026	643.5000	Traffic Control	EACH	1.000	0.620	0.380

FINISHING ROADWAY		
CATEGORY	PROJECT	213.0100 EACH
0010	1050-11-80	1

MOBILIZATION		
CATEGORY	PROJECT	619.1000 EACH
0010	1050-11-80	0.62

PROTECTIVE SURFACE TREATMENT RESEAL					
CATEGORY	STRUCTURE ID	HWY ON	FEATURE UNDER	TYPE	502.3215.S SY
0020	B-09-0019	CTH X	STH 29	BRIDGE COLUMNS	125.6
0030	B-09-0020	STH 29 EB	CTH X	BRIDGE DECK	818.3
0040	B-09-0022	CTH X	STH 29	BRIDGE COLUMNS	125.6
0050	B-09-0030	CTH X/ 37TH AVE	STH 29	BRIDGE COLUMNS	94.2
0060	B-09-0031	STH 29 WB	STILLSON CREEK	BRIDGE DECK	571.3
0070	B-09-0032	160TH ST	STH 29	BRIDGE COLUMNS	94.2
0080	B-09-0033	50TH AVE	STH 29	BRIDGE COLUMNS	94.2
0090	B-09-0034	CTH J	STH 29	BRIDGE COLUMNS	94.2
0100	B-09-0035	STH 29 WB	190TH ST	BRIDGE DECK	494.9
0110	B-09-0036	195TH ST	STH 29	BRIDGE COLUMNS	94.2
0120	B-09-0037	CTH K/STILLSON RD	STH 29	BRIDGE COLUMNS	94.2
0130	B-09-0038	STH 29 WB	PAINT CREEK	BRIDGE DECK	486.6
0140	B-09-0039	220TH ST	STH 29	BRIDGE COLUMNS	94.2
0150	B-09-0171	STH 29 EB	STILLSON CREEK	BRIDGE DECK	776.8
0160	B-09-0174	STH 29 EB	190TH ST	BRIDGE DECK	467.2
0170	B-09-0175	STH 29 EB	PAINT CREEK	BRIDGE DECK	510.7
0180	B-09-0176	CTH XX	STH 29	BRIDGE COLUMN	31.4
0190	B-09-0177	STH 29 WB	CTH X	BRIDGE DECK	812.7
0200	B-09-0179	STH 27	STH 29	BRIDGE COLUMNS	125.6
0210	B-09-0189	CTH D	STH 29	BRIDGE COLUMNS	167.5
0220	B-09-0190	STH 29 WB	CTH H	BRIDGE DECK	683.7
0230	B-09-0191	STH 29 EB	CTH H	BRIDGE DECK	683.7
0240	B-09-0206	STH 29 WB	ELK CREEK	BRIDGE DECK	357.7
0250	B-09-0207	STH 29 EB	ELK CREEK	BRIDGE DECK	399.0
0260	B-09-0208	CTH T	STH 29	BRIDGE COLUMNS	62.8
0270	B-09-0209	STH 29 WB	CHIPPEWA RIVER	BRIDGE DECK	4006.9
0280	B-09-0210	STH 29 EB	CHIPPEWA RIVER	BRIDGE DECK	3306.3
0290	B-09-0211	110TH ST	STH 29	BRIDGE COLUMNS	31.4
0300	B-09-0212	120TH ST	STH 29	BRIDGE COLUMNS	62.8
0310	B-09-0213	STH 29 WB	UP RR	BRIDGE DECK	1172.6
0320	B-09-0214	STH 29 EB	UP RR	BRIDGE DECK	929.8
0330	B-09-0215	STH 29 WB	STH 124	BRIDGE DECK	1272.2
0340	B-09-0216	STH 29 EB	STH 124	BRIDGE DECK	992.1
0350	B-09-0217	STH 29 WB	USH 53	BRIDGE DECK	2349.2
0360	B-09-0218	STH 29 EB	USH 53	BRIDGE DECK	2157.8
0370	B-09-0219	130TH ST	STH 29	BRIDGE COLUMNS	31.4
0380	B-09-0220	CTH P	STH 29	BRIDGE COLUMNS	31.4
0390	B-09-0221	40TH AVE	STH 29	BRIDGE COLUMNS	31.4
0400	B-09-0222	50TH AVE	STH 29	BRIDGE COLUMNS	31.4
0410	B-09-0223	SEYMOUR CRAY SR BLVD	STH 29	BRIDGE COLUMNS	62.8
0420	B-09-0225	SB SEYMOUR CRAY SR BLVD	STH 29	BRIDGE COLUMNS	31.4
0430	B-09-0226	NB SEYMOUR CRAY SR BLVD	STH 29	BRIDGE COLUMNS	31.4
0440	B-09-0241	CTH F	STH 29	BRIDGE COLUMNS	31.4
0450	B-09-0242	90TH ST	STH 29	BRIDGE COLUMNS	41.9
0460	B-09-0243	STH 29 WB	103RD ST	BRIDGE DECK	486.7
0470	B-09-0244	STH 29 EB	103RD ST	BRIDGE DECK	486.7
0480	B-09-0245	STH 29 OFF RAMP	STH 124	BRIDGE DECK	657.9
0490	B-17-0059	IH 94 WB	USH12/STH29	BRIDGE COLUMNS	188.4
0500	B-17-0060	IH 94 EB	USH12/STH29	BRIDGE COLUMNS	188.4
0510	B-17-0116	USH12/STH29	UP RR	BRIDGE DECK	911.9
0520	B-17-0163	USH12/STH29	UP RR	BRIDGE DECK	812.0
TOTAL					28,698.1

TRAFFIC CONTROL									
CATEGORY	PROJECT	643.0300	643.0420	643.0705	643.0715	643.0800	643.0900	643.1051	643.5000
		DRUMS DAYS	BARRICADES TYPE III DAYS	LIGHTS TYPE A DAYS	LIGHTS TYPE C DAYS	ARROW BOARDS DAYS	SIGNS DAYS	SIGNS PCMS DAYS	TRAFFIC CONTROL EACH
0010	1050-11-80	3300	70	135	980	135	980	25	0.62

<u>FINISHING ROADWAY</u>		
<u>CATEGORY</u>	<u>PROJECT</u>	<u>213.0100 EACH</u>
0010	1050-11-81	1

<u>MOBILIZATION</u>		
<u>CATEGORY</u>	<u>PROJECT</u>	<u>619.1000 EACH</u>
0010	1050-11-81	0.38

<u>INLET PROTECTION TYPE B</u>				
<u>CATEGORY</u>	<u>STRUCTURE</u>	<u>ROADWAY</u>	<u>LOCATION</u>	<u>628.7010 EACH</u>
0040	B-10-0020	STH 29 EB	POPPLE RIVER	1
0060	B-10-0023	STH 29 EB	WIS CENT.RR	6
0170	B-10-0173	STH 29 WB	CTH D	1
0180	B-10-0174	STH 29 WB	WIS CENT. RR	4
TOTAL				12

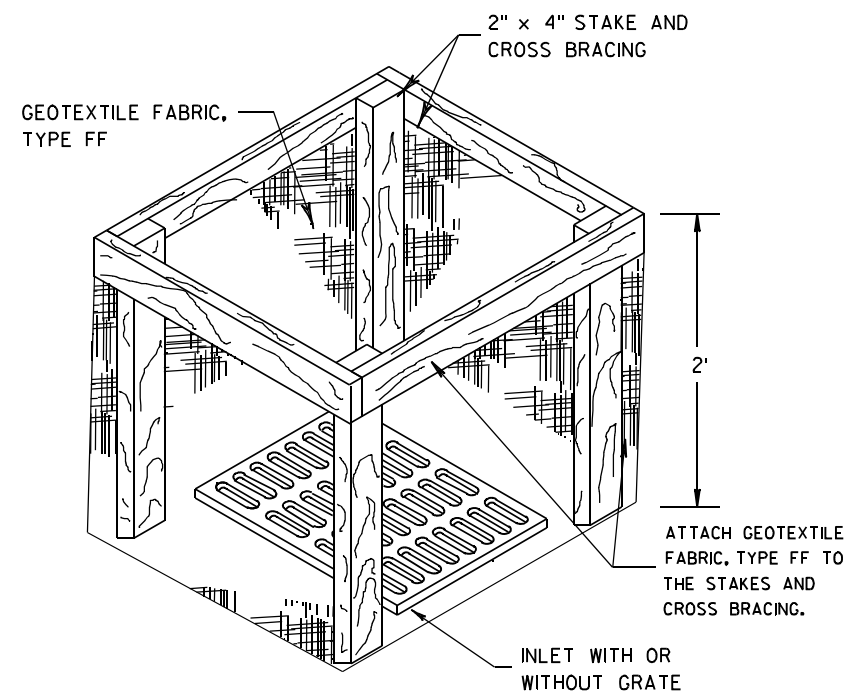
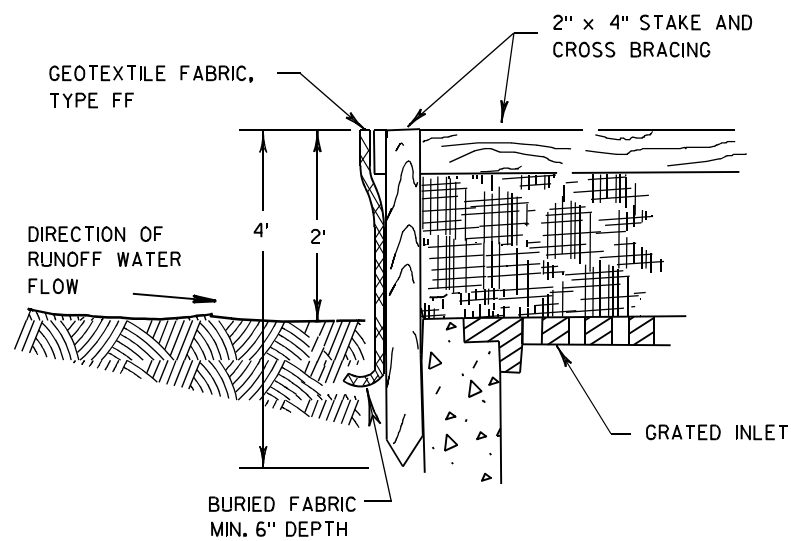
NOTE: INLET PROTECTION INCLUDES TWO INSTALLATIONS PER INLET. FIRST INSTALLATIONS IS WITH TYPE FF FABRIC DURING DECK PREPARATION AND CLEANING ACTIVITIES. SECOND INSTALLATION IS WITH IMPERVIOUS FABRIC DURING

PROTECTIVE SURFACE TREATMENT RESEAL					
CATEGORY	STRUCTURE ID	HWY ON	FEATURE UNDER	TYPE	502.3215.S SY
0020	B-10-0017	STH 29 EB	DILL CREEK	BRIDGE DECK	284.7
0030	B-10-0019	STH 29 EB/STH 73 SB	BLACK RIVER	BRIDGE DECK	1260.0
0040	B-10-0020	STH 29 EB	POPPLE RIVER	BRIDGE DECK	843.2
0050	B-10-0022	STH 29 EB	CTH D	BRIDGE DECK	632.7
0060	B-10-0023	STH 29 EB	WC RR	BRIDGE DECK	810.3
0070	B-10-0143	STH 29 WB	N FK EAU CLAIRE RIVER	BRIDGE DECK	647.3
0080	B-10-0144	STH 29 WB	STH 73 NB	BRIDGE DECK	677.4
0090	B-10-0145	STH 29 EB	STH 73 NB	BRIDGE DECK	677.4
0100	B-10-0146	STH 29 EB	N FK EAU CLAIRE RIVER	BRIDGE DECK	647.3
0110	B-10-0149	STH 29 EB	N FK POPPLER RIVER	BRIDGE DECK	564.2
0120	B-10-0151	HILINE DRIVE	STH 29	BRIDGE COLUMNS	31.4
0130	B-10-0168	STH 29 WB/STH 73 NB	S FK EAU CLAIRE RIVER	BRIDGE DECK	494.4
0140	B-10-0169	STH 29 EB/STH 73 SB	S FK EAU CLAIRE RIVER	BRIDGE DECK	494.4
0150	B-10-0170	STH 29 WB/STH 73 NB	BLACK RIVER	BRIDGE DECK	1551.8
0160	B-10-0171	STH 29 WB	POPPLE RIVER	BRIDGE DECK	930.2
0170	B-10-0173	STH 29 WB	CTH D	BRIDGE DECK	808.8
0180	B-10-0174	STH 29 WB	WC RR	BRIDGE DECK	916.8
0190	B-10-0175	STH 29 WB	N FK POPPLE RIVER	BRIDGE DECK	564.2
0200	B-10-0176	STH 29 WB	DILL CREEK	BRIDGE DECK	321.1
0210	B-10-0177	STH 73 NB	STH 29	BRIDGE COLUMNS	125.6
0220	B-10-0178	CARDINAL AVE	STH 29	BRIDGE COLUMNS	41.9
0230	B-10-0179	CTH E	STH 29	BRIDGE COLUMNS	83.7
TOTAL					13408.8

TRAFFIC CONTROL									
		643.0300	643.0420	643.0705	643.0715	643.0800	643.0900	643.1051	643.5000
		DRUMS	BARRICADES	LIGHTS	LIGHTS	ARROW	SIGNS	SIGNS	TRAFFIC
CATEGORY	PROJECT	DAYS	TYPE III	TYPE A	TYPE C	BOARDS	SIGNS	PCMS	CONTROL
0010	1050-11-81	2200	50	90	650	90	650	25	0.38

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15D12-07A	TRAFFIC CONTROL, LANE CLOSURE
15D12-07B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION
15D15-04A	TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-04B	TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-04C	TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-04D	TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE
15D16-04	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



INLET PROTECTION, TYPE A

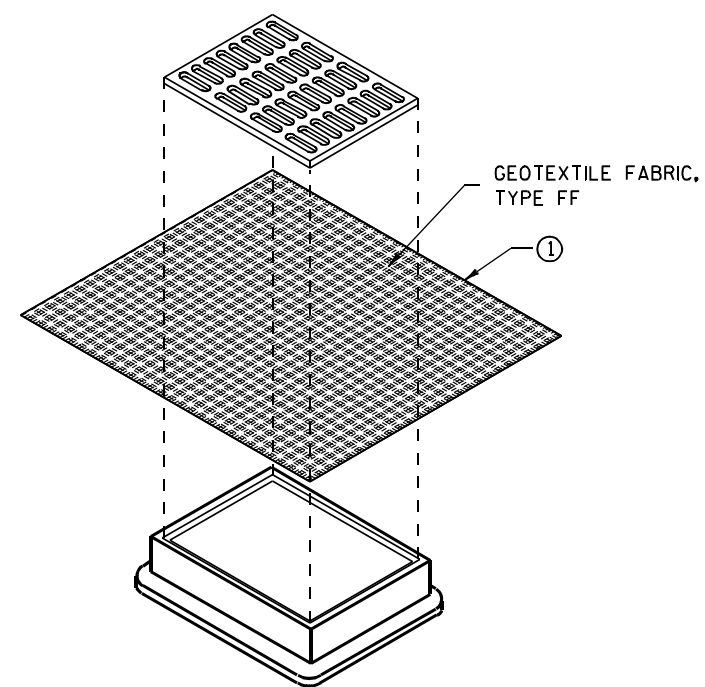
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

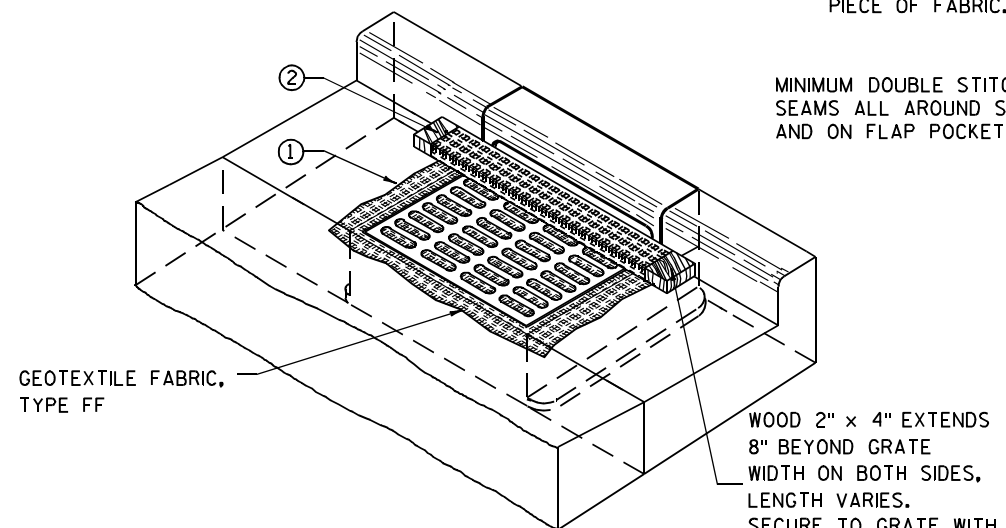
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

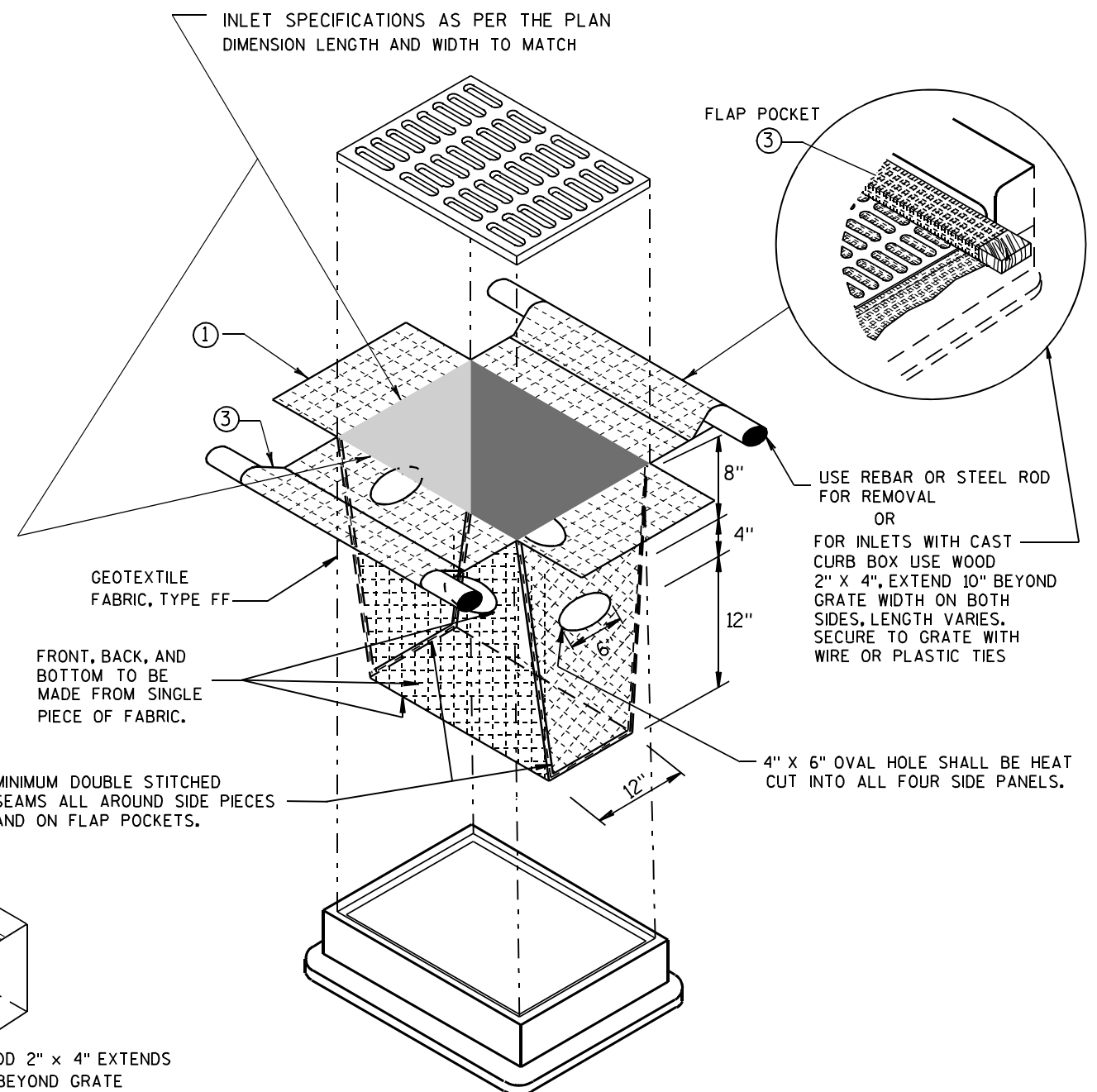
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



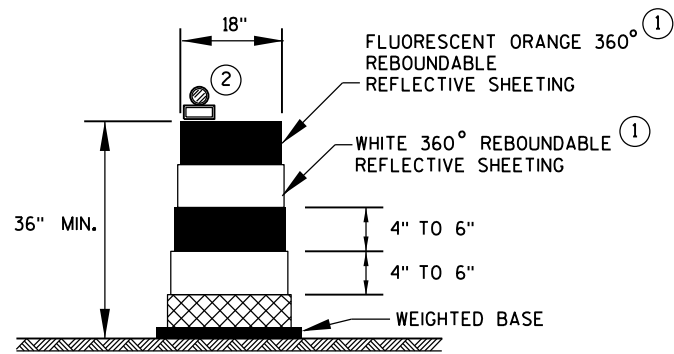
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

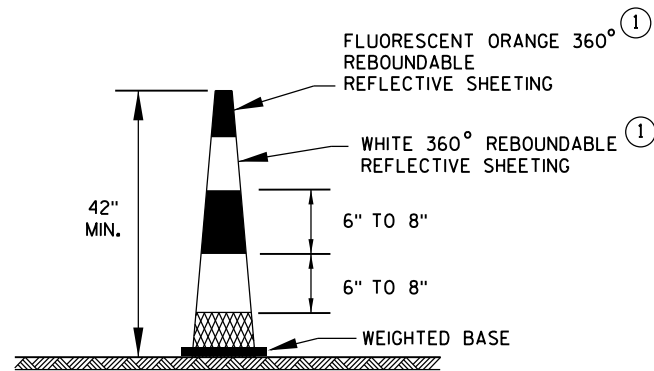
**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Cannestra
DATE
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER



DRUM

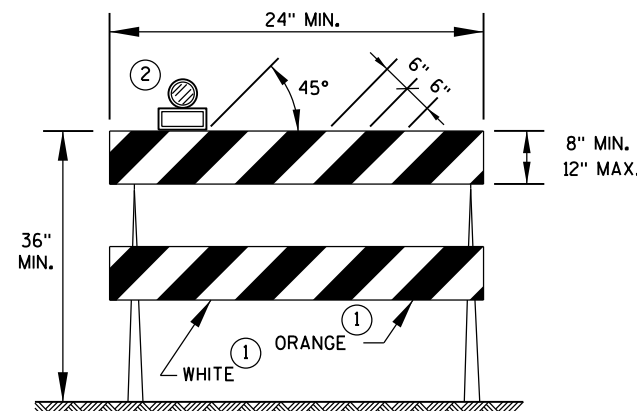


42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS

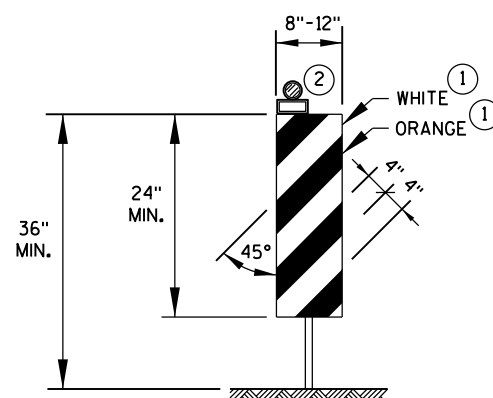
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



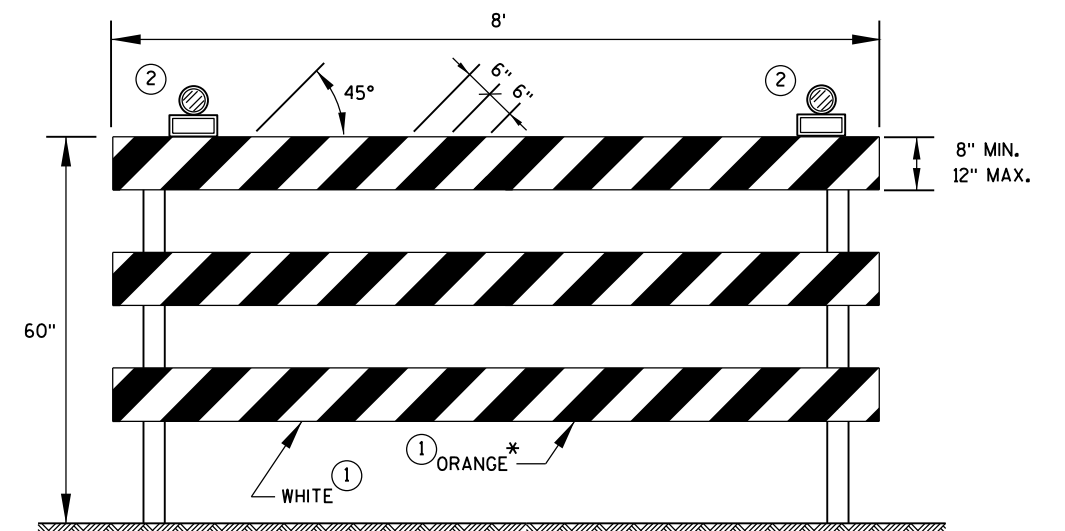
TYPE 2 BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE 3 BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017
DATE

FHWA

/S/ Andrew Heidtke
WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

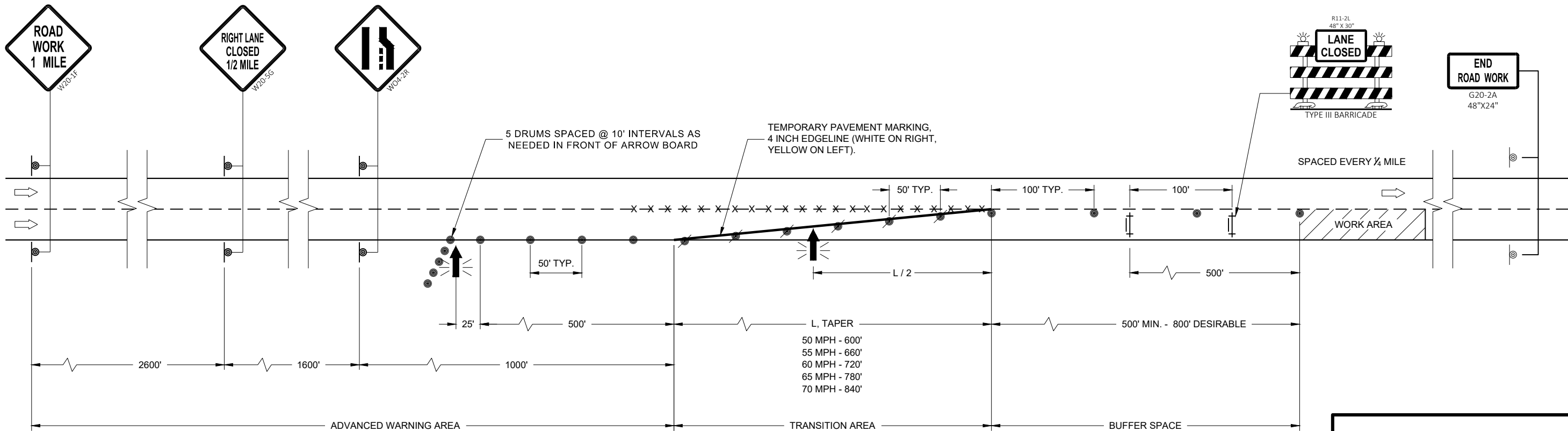
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS

NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- WORK AREA
- FLASHING ARROW BOARD



TRAFFIC CONTROL
LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

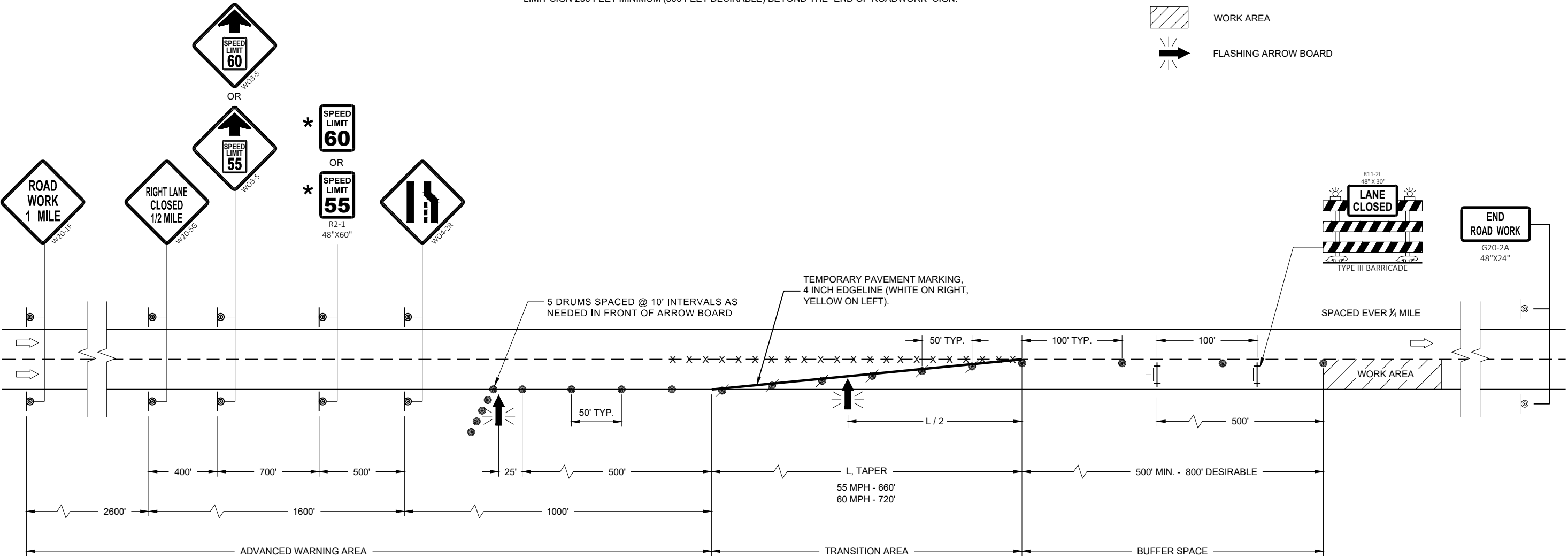
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

* A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. PLACE A SPEED LIMIT SIGN A MINIMUM OF EVERY 3 MILES. INCLUDE A RESUME SPEED LIMIT SIGN 200 FEET MINIMUM (500 FEET DESIRABLE) BEYOND THE "END OF ROADWORK" SIGN.

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- WORK AREA
- FLASHING ARROW BOARD

6



6

SDD 15D12 - 07b

TRAFFIC CONTROL,
LANE CLOSURE,
SPEED REDUCTION

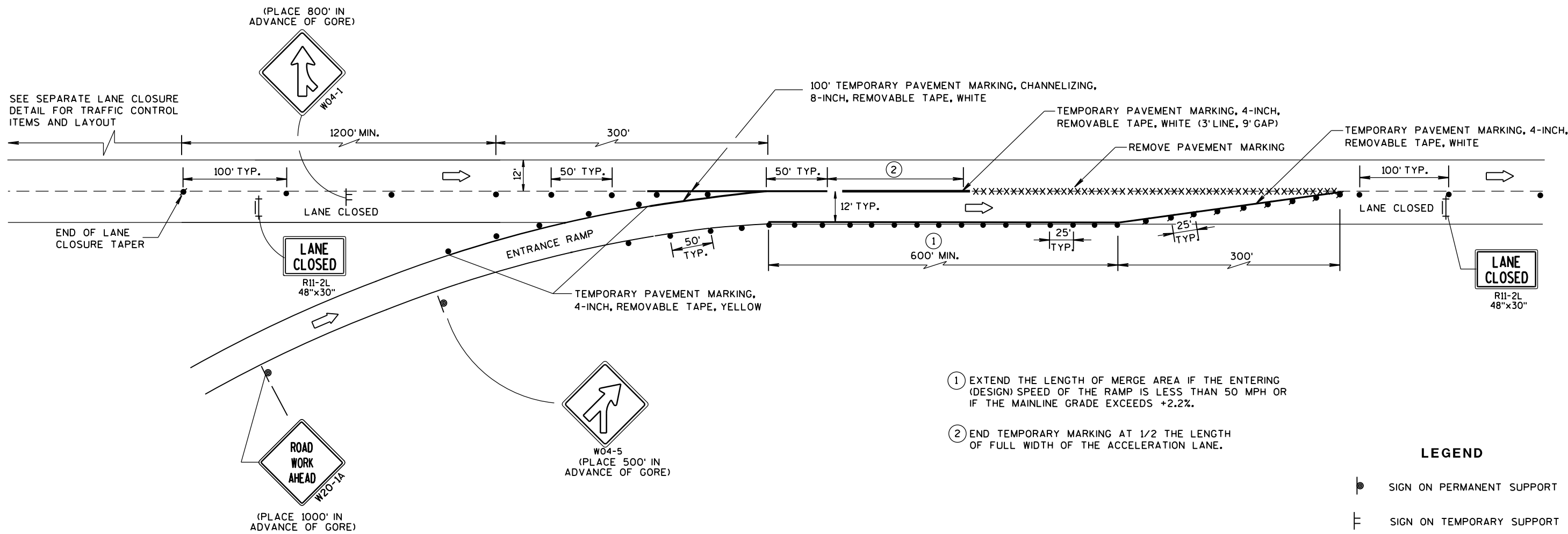
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

SDD 15D12 - 07b

TRAFFIC CONTROL, ENTRANCE RAMP WITHIN RIGHT LANE CLOSURE



PARALLEL ENTRANCE RAMP

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

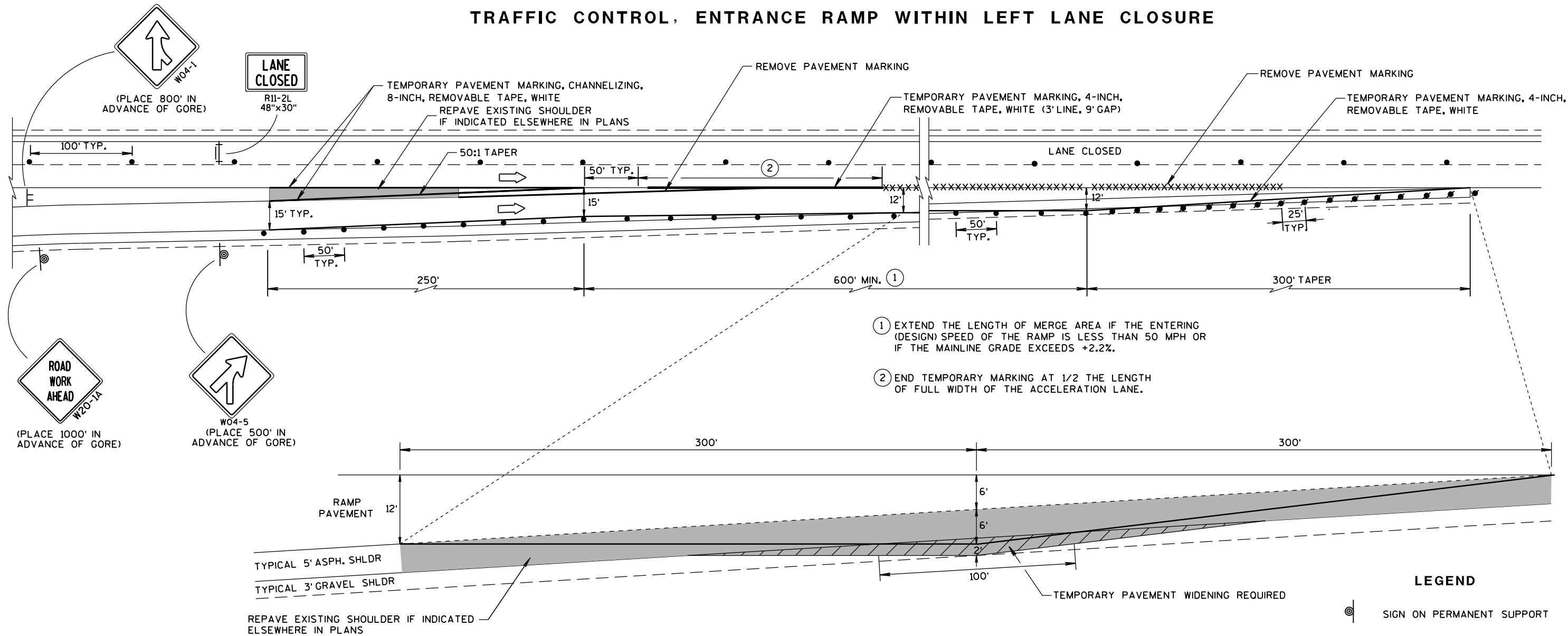
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

LEGEND

- SIGN ON PERMANENT SUPPORT
- ├ SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ├ TYPE III BARRICADE WITH ATTACHED SIGN
- XXXXX REMOVING PAVEMENT MARKING
- ➡ DIRECTION OF TRAFFIC

TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED Sept., 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LEFT LANE CLOSURE



TEMPORARY PAVEMENT DETAIL

(EXISTING RAMP DIMENSIONS MAY VARY. ADJUST TEMPORARY PAVEMENT ACCORDINGLY)

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⌄ SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ⌄ TYPE III BARRICADE WITH ATTACHED SIGN
- XXXXX REMOVING PAVEMENT MARKING
- ➡ DIRECTION OF TRAFFIC

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"W0" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

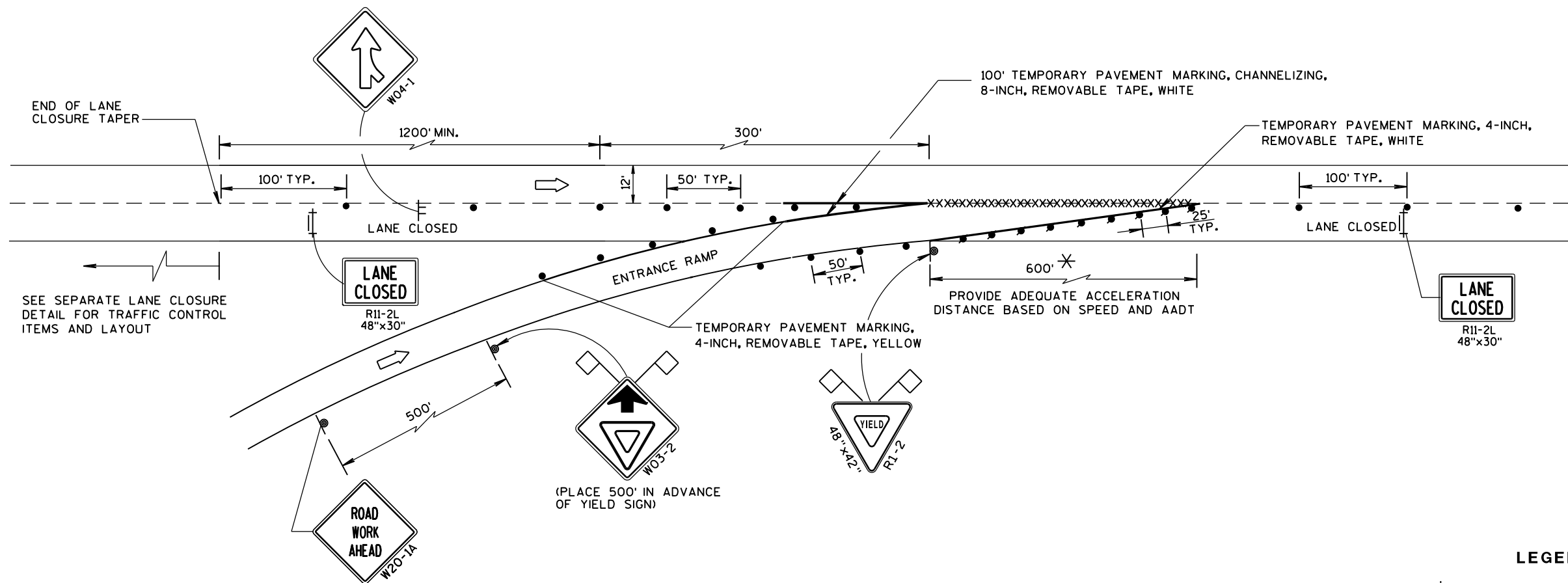
REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2017 /S/ Andrew Heldtke
DATE WORK ZONE ENGINEER
FHWA



**TAPERED ENTRANCE RAMP
WITHIN RIGHT LANE CLOSURE**

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

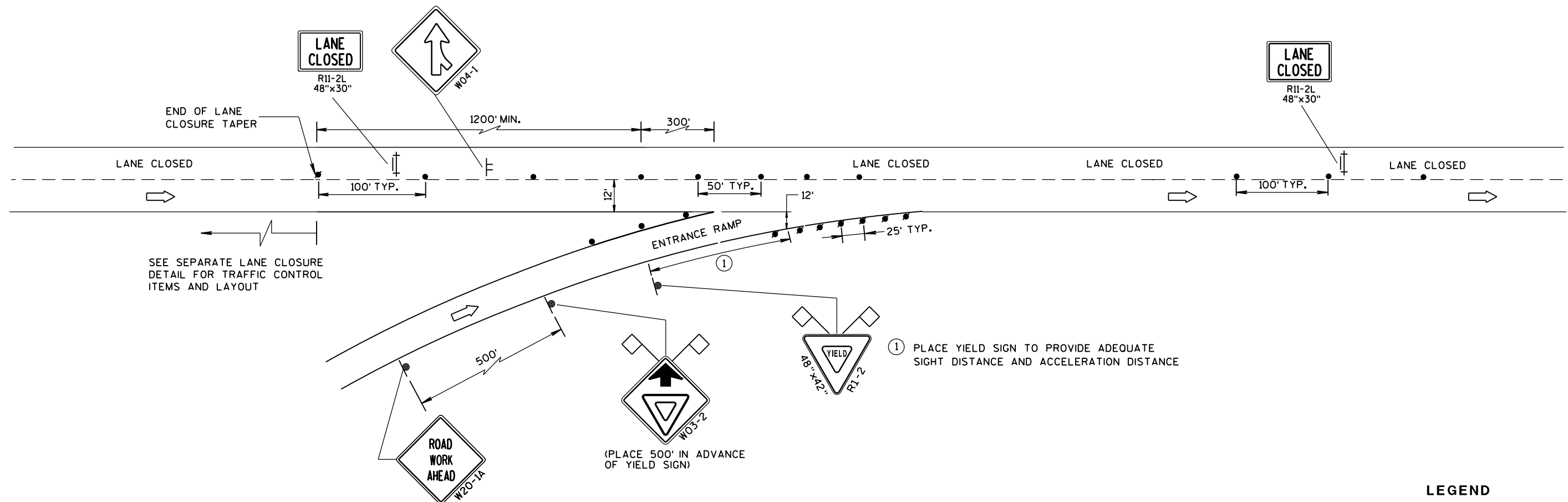
REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

✱ CONSULT WITH REGIONAL WORK ZONE ENGINEER IF NEED TO REDUCE LENGTH EXISTS.

LEGEND	
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
	REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
	TYPE III BARRICADE WITH ATTACHED SIGN
	FLAGS, 16" x 16" MIN., (ORANGE)
	DIRECTION OF TRAFFIC

TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED Sept., 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



TAPERED ENTRANCE RAMP WITHIN LEFT LANE CLOSURE

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- TYPE III BARRICADE WITH ATTACHED SIGN
- FLAGS, 16" x 16" MIN., (ORANGE)
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,
TAPERED ENTRANCE RAMP
WITHIN LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

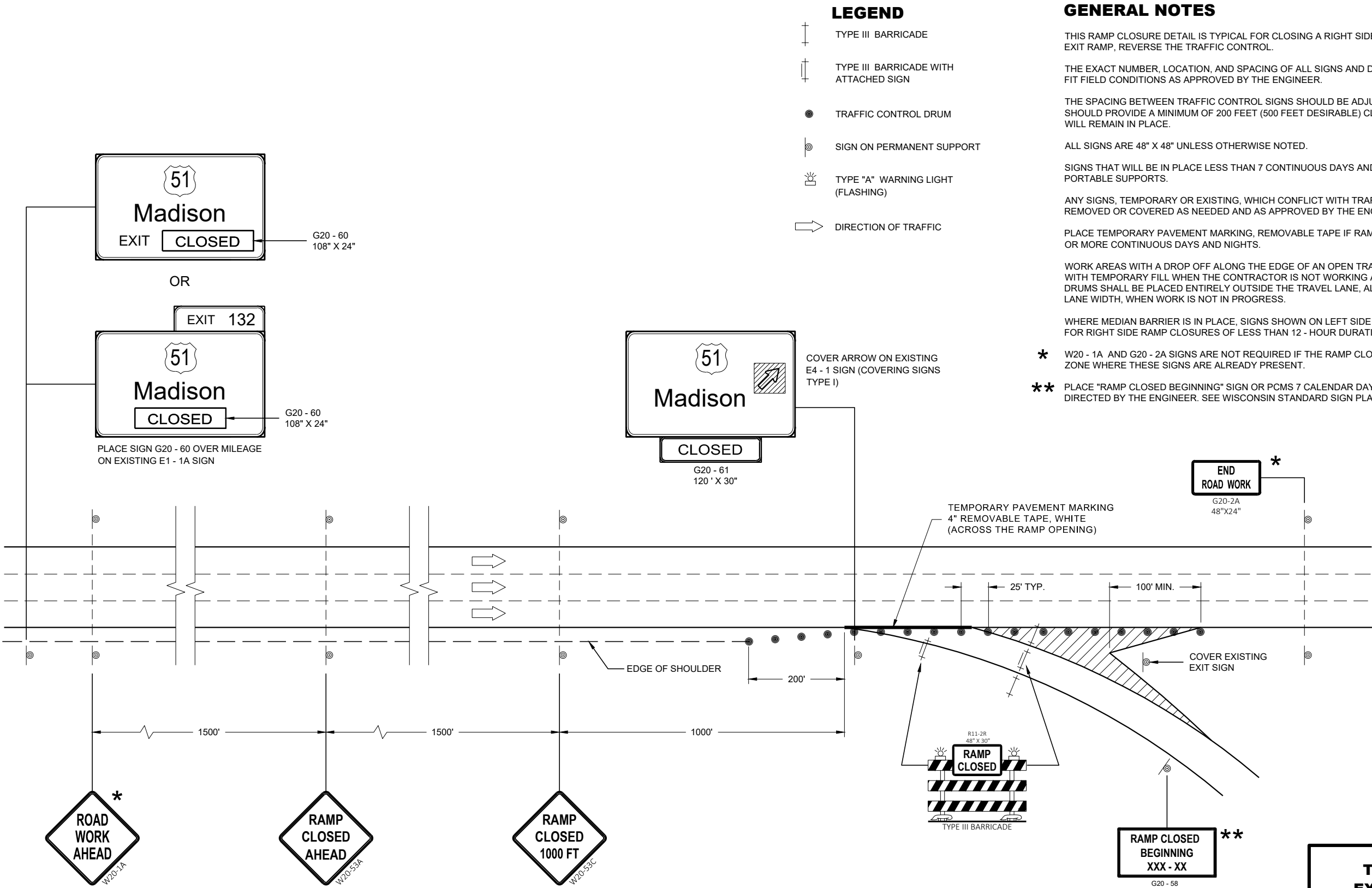
Sept., 2017

DATE

FHWA

/S/ Andrew Heidtke

WORK ZONE ENGINEER



LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF RAMP CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROP OFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12 - HOUR DURATION.

* W20 - 1A AND G20 - 2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

** PLACE "RAMP CLOSED BEGINNING" SIGN OR PCMS 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.

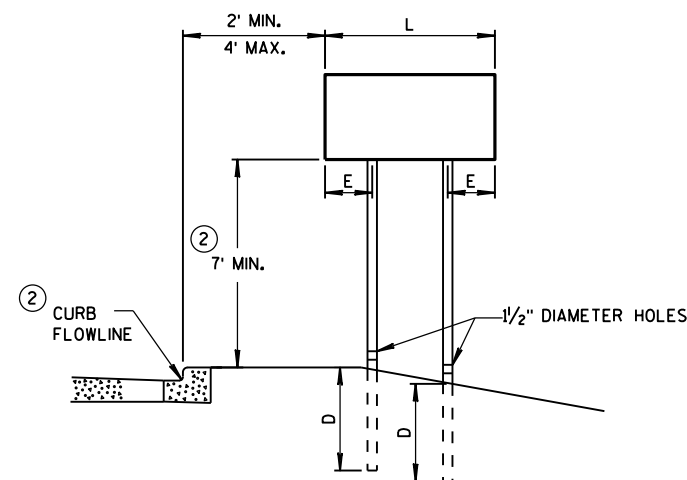
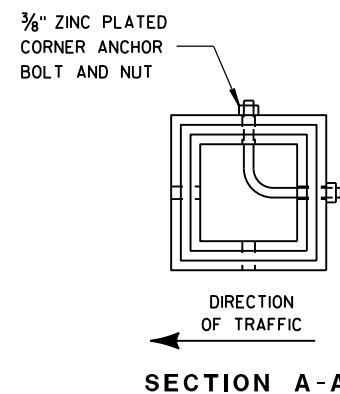
TRAFFIC CONTROL EXIT RAMP CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS LARGER THAN 27 SQ.FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

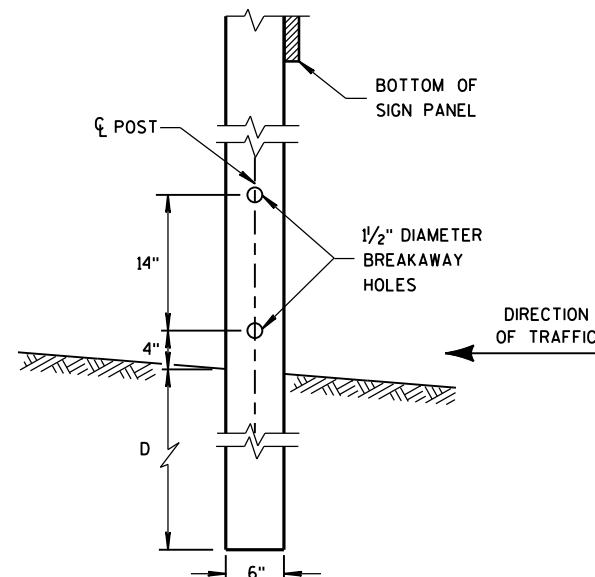


URBAN AREA

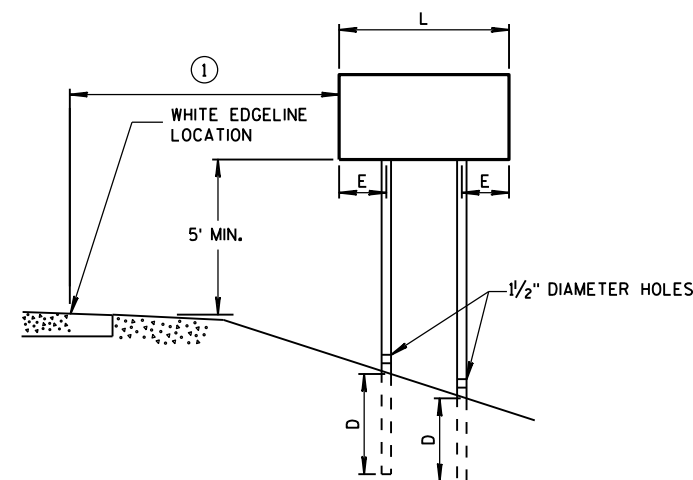
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST
EMBEDMENT DEPTH

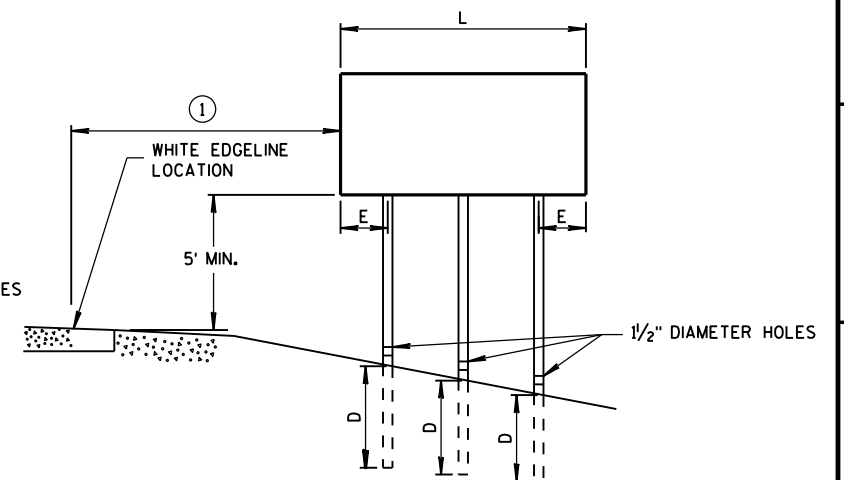
AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



4"x6" WOOD POST MODIFICATION



RURAL AREA



GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

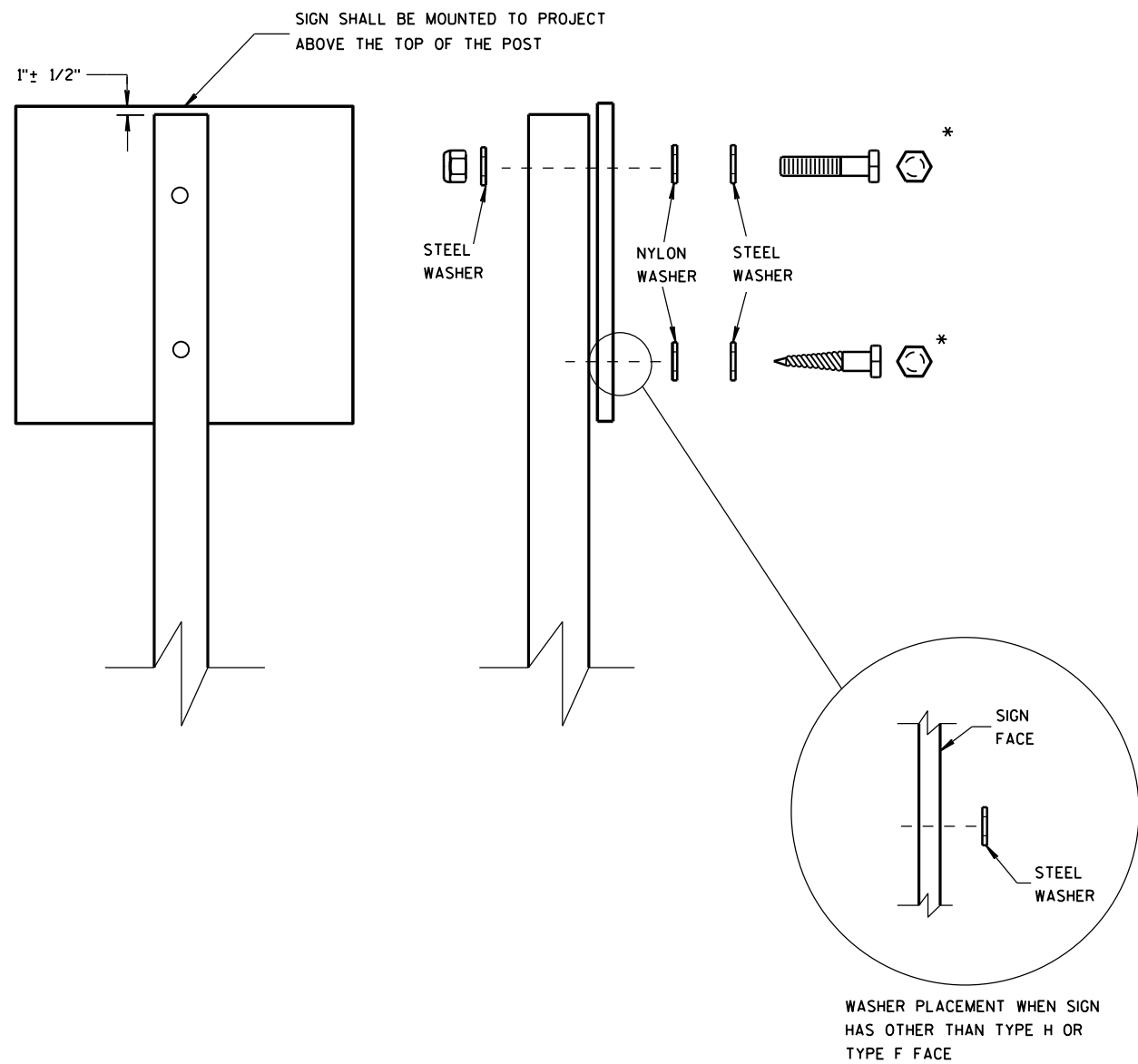
4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

SEE NOTE (3)

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

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NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" x 3"
 - MACHINE BOLTS - 5/16" x 6-1/2" OR 7" LENGTH W/ NUTS

- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" x 3-1/4" LENGTH W/ NUTS
 - RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

- WASHERS (ALL POSTS) -
- 1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL
 - 1-1/4" O.D. x 3/8" I.D. x .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heldtke WORK ZONE ENGINEER
FHWA	

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

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