

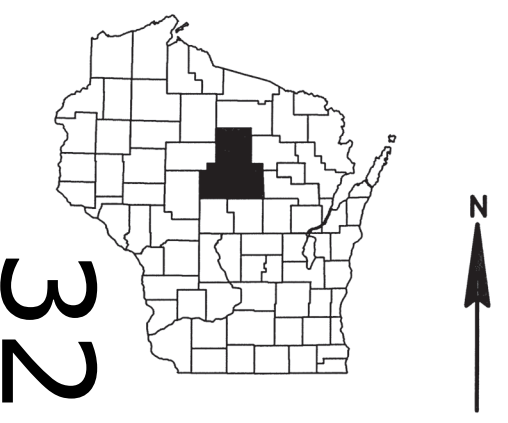
WIS PROJECT ID: 1009-47-61 COUNTY: NORTH CENTRAL REGION WIDE WITH: 1009-47-62

MAY 2019

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Gross Sections

TOTAL SHEETS = 70








DESIGN DESIGNATION*

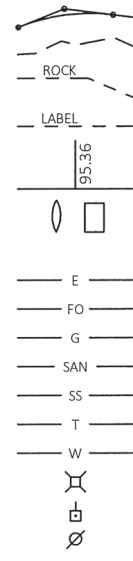
A.A.D.T.	2016	=	STH 17 1,800
A.A.D.T.	VAR.	=	STH 153 VAR
A.A.D.T.	2013	=	STH 52 VAR.
D.H.V.		=	
D.D.		=	
T.		=	
DESIGN SPEED		=	
ESALS		=	

*SEE GENERAL NOTES SHEET FOR
STRUCTURE AADT SUMMARY TABLE

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

<u>PROFILE</u>	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
<u>UTILITIES</u>	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

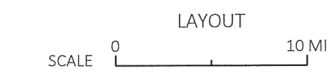
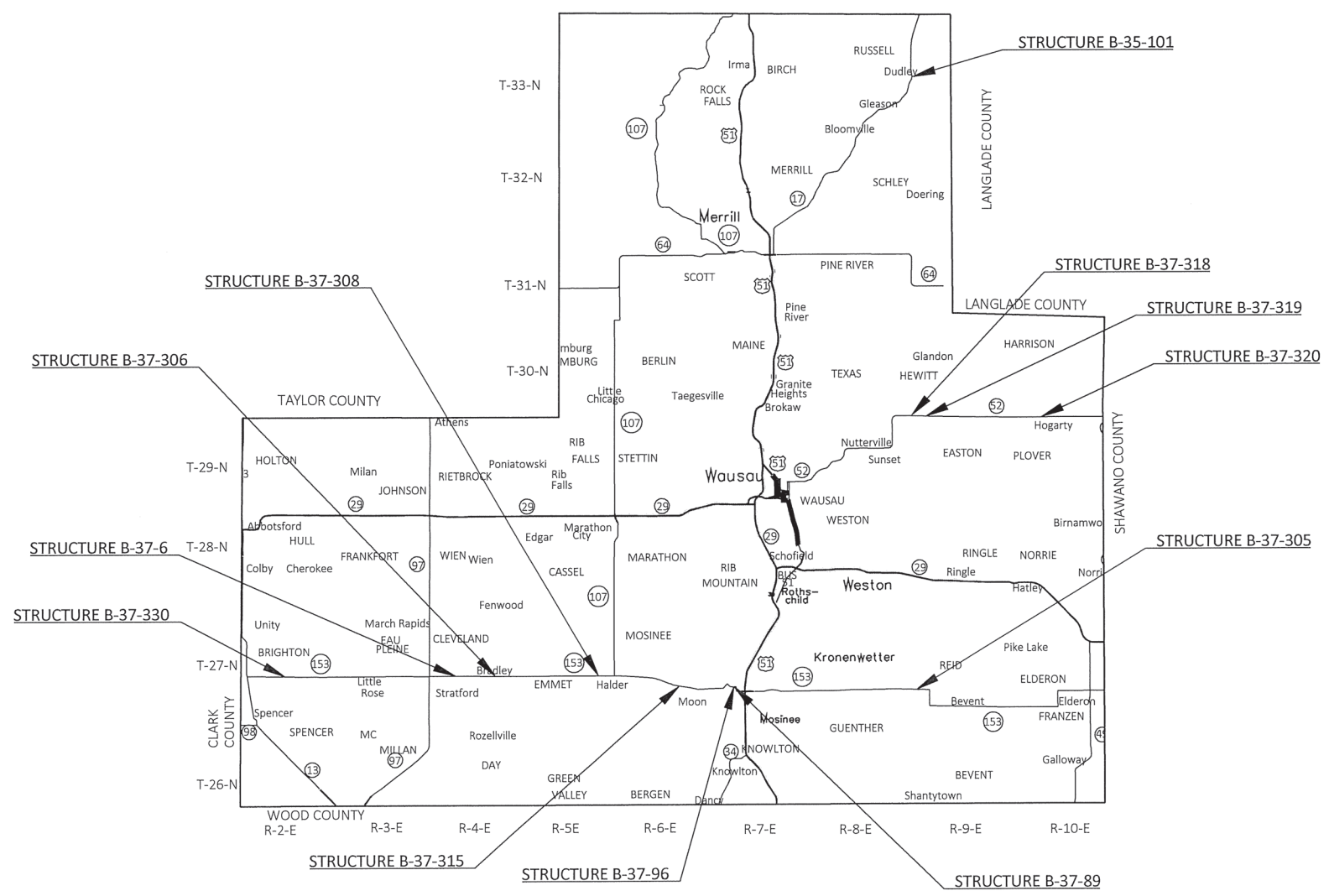
PLAN OF PROPOSED IMPROVEMENT

REGIONWIDE BRIDGE POLYMER OVERLAYS

LINCOLN/MARATHON BRIDGES

VAR HWY
NORTH CENTRAL REGION WIDE

STATE PROJECT NUMBER
1009-47-61



TOTAL NET LENGTH OF CENTERLINE = STH 153: 0 MILES, STH 52: 0 MILES, STH 17: 0 MILES

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1009-47-61	WISC 2019301	1

ORIGINAL PLANS PREPARED BY

DATE: 1/24/19

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	
Surveyor	N/A
Designer	SEH
Project Manager	WENDY ARNESON
Regional Examiner	CHERYL SIMON
Regional Supervisor	MICHAEL KRETSCHMER

APPROVED FOR THE DEPARTMENT

DATE: 1/25/19

E

STRUCTURES INCLUDED IN PROJECT

LINCOLN COUNTY
STH 17 OVER THE PRAIRIE RIVER (B-35-101)

MARATHON COUNTY
STH 153 OVER AN UNNAMED BRANCH OF THE BIG EAU PLEINE RIVER (B-37-6)
STH 153 OVER THE WISCONSIN RIVER, WISCONSIN CENTRAL RAILROAD, AND DEPOT STREET (B-37-89)
STH 153 OVER THE WISCONSIN RIVER (B-37-96)
STH 153 OVER THE LITTLE EAU CLAIRE RIVER (B-37-305)
STH 153 OVER THE BIG EAU PLEINE RIVER (B-37-306)
STH 153 OVER THE FREEMAN CREEK (B-37-308)
STH 153 OVER THE HOG CREEK (B-37-315)
STH 52 OVER THE WEST BRANCH OF THE BIG SANDY CREEK (B-37-318)
STH 52 OVER THE BIG SANDY CREEK (B-37-319)
STH 52 OVER THE EAU CLAIRE RIVER (B-37-320)
STH 153 OVER THE LITTLE EAU PLEINE RIVER (B-37-330)

STRUCTURE AADT SUMMARY TABLE		
STRUCTURE I.D.	YEAR	AADT
B-35-101	2016	1,800
B-37-6	2016	3,500
B-37-89	2016	12,500
B-37-96	2013	11,700
B-37-305	2016	1,300
B-37-306	2016	3,000
B-37-308	2004	3,900
B-37-315	2016	3,000
B-37-318	2013	2,300
B-37-319	2013	2,300
B-37-320	2013	1,845
B-37-330	2016	1,300

GENERAL NOTES

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

SIGNAL TIMING INFORMATION

STRUCTURE #	STAGE #	YELLOW			ALL RED			GREEN			TOTAL SPLIT		
		Φ1	Φ2	Φ4	Φ1	Φ2	Φ4	Φ1	Φ2	Φ4	Φ1	Φ2	Φ4
		NB	SB	WB	NB	SB	WB	NB	SB	WB	NB	SB	WB
B-35-101	1	2.8	2.8	3.6	15.8	15.8	4.8	24.4	24.4	10.6	43.0	43.0	19.0
	2	2.8	2.8	3.6	11.9	11.9	3.8	25.3	25.3	10.6	40.0	40.0	18.0

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		EB	WB	EB	WB	EB	WB	EB	WB
B-37-0006	ALL	2.8	2.8	11.9	11.9	25.3	25.3	40.0	40.0

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		EB	WB	EB	WB	EB	WB	EB	WB
B-37-0306	ALL	2.8	2.8	21.0	21.0	21.2	21.2	45.0	45.0

STRUCTURE #	STAGE #	YELLOW			ALL RED			GREEN			TOTAL SPLIT		
		Φ1	Φ2	Φ4	Φ1	Φ2	Φ4	Φ1	Φ2	Φ4	Φ1	Φ2	Φ4
		EB	WB	SB	EB	WB	SB	EB	WB	SB	EB	WB	SB
B-37-0308	ALL	2.8	2.8	5.0	16.9	16.9	3.0	26.3	26.3	10.0	46.0	46.0	18.0


STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		EB	WB	EB	WB	EB	WB	EB	WB
B-37-0315	ALL	2.8	2.8	12.6	12.6	24.6	24.6	40.0	40.0

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		EB	WB	EB	WB	EB	WB	EB	WB
B-37-0320	ALL	2.8	2.8	15.5	15.5	21.7	21.7	40.0	40.0

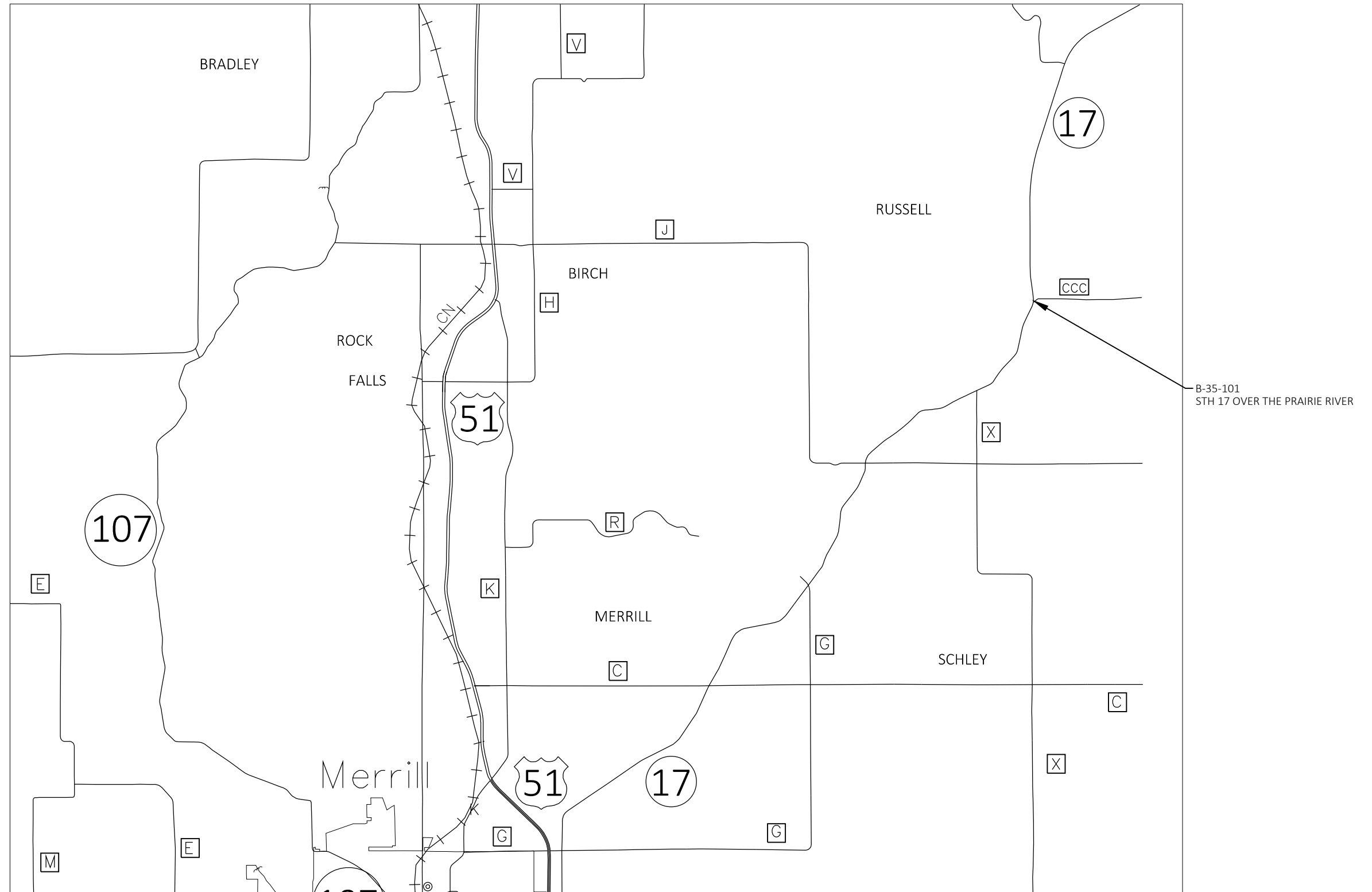
WDNR CONTACT - LINCOLN COUNTY
107 SUTLIFF AVENUE
RHINELANDER, WI 54501
TELEPHONE: 715.365.8916
ATTENTION: WENDY HENNIGES
EMAIL: WENDY.HENNIGES@WISCONSIN.GOV

WDNR CONTACT - MARATHON COUNTY
473 GRIFFITH DRIVE
WISCONSIN RAPIDS, WI 54494
TELEPHONE: 715.213.6571
ATTENTION: CASEY JONES
EMAIL: CASEY.JONES@WISCONSIN.GOV



Dial  or (800)242-8511
www.DiggersHotline.com

PROJECT OVERVIEW - STH 17 BRIDGE



PROJECT NO: 1009-47-61

HWY: VAR HWY

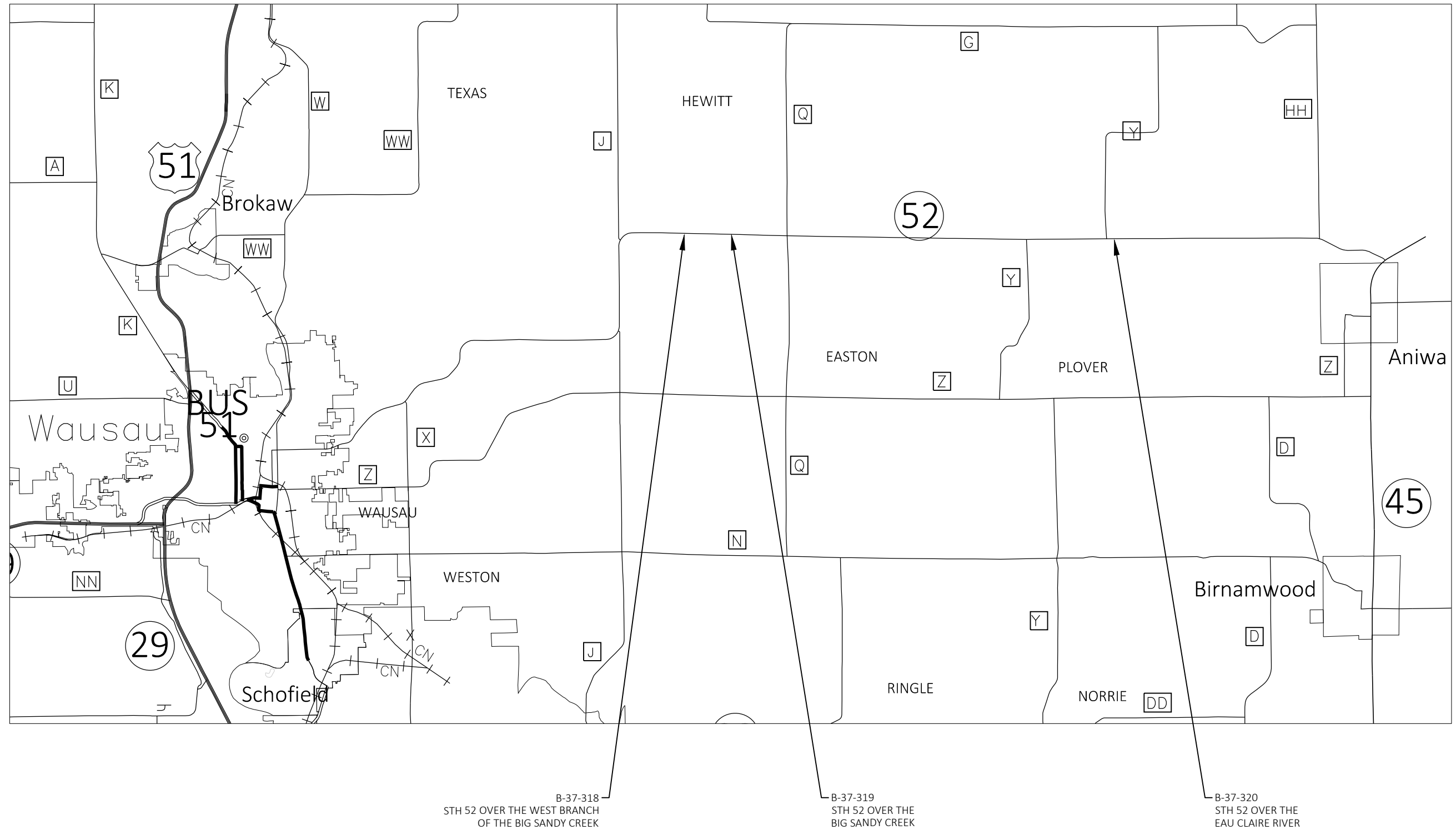
COUNTY: NORTH CENTRAL REGION WIDE

PROJECT OVERVIEW

SHEET

E

PROJECT OVERVIEW - STH 52 BRIDGES



PROJECT NO: 1009-47-61

HWY: VAR HWY

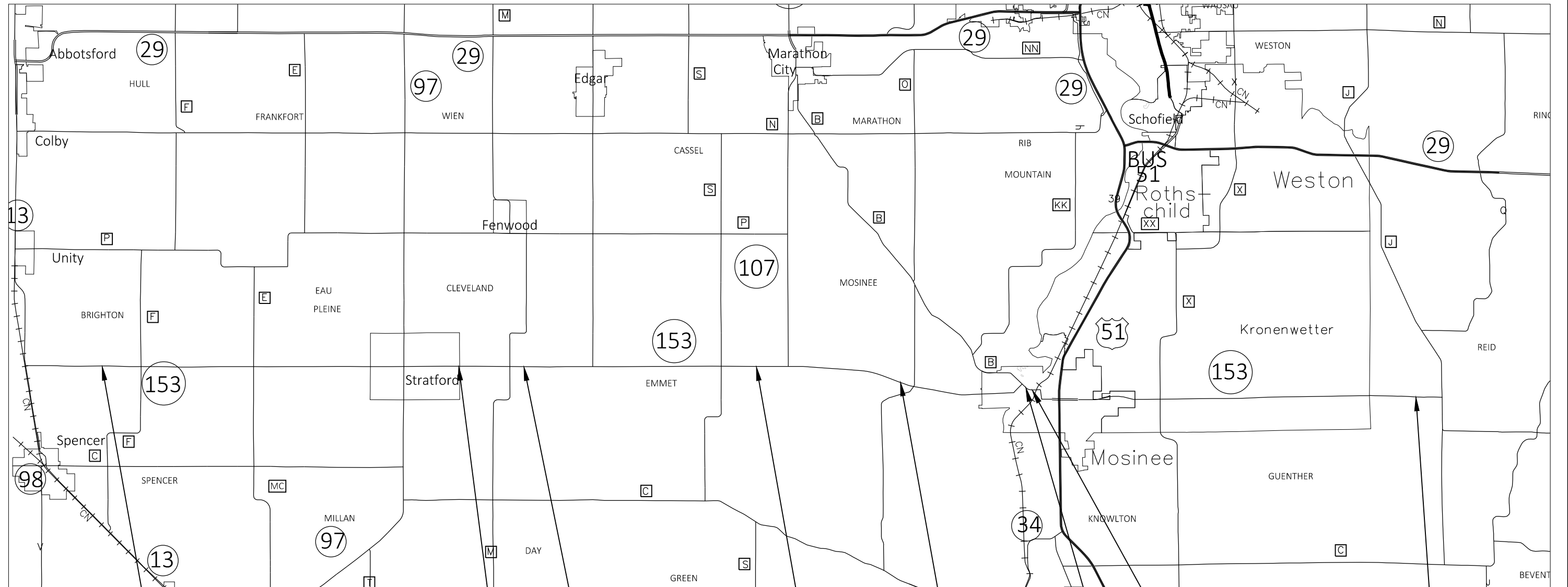
COUNTY: NORTH CENTRAL REGION WIDE

PROJECT OVERVIEW

SHEET

E

PROJECT OVERVIEW - STH 153 BRIDGES



B-37-330
STH 153 OVER THE
LITTLE EAU PLEINE RIVER

B-37-6
STH 153 OVER AN
UNNAMED BRANCH OF
THE BIG EAU PLEINE RIVER

B-37-306
STH 153 OVER THE
BIG EAU PLEINE RIVER

B-37-308
STH 153 OVER THE
FREEMAN CREEK

B-37-315
STH 153 OVER
THE HOG CREEK

B-37-96
STH 153 OVER THE
WISCONSIN RIVER

B-37-89
STH 153 OVER THE WISCONSIN
CENTRAL RAILROAD, AND DEPOT STREET

B-37-305
STH 153 OVER THE
LITTLE EAU CLAIRE RIVER

PROJECT NO: 1009-47-61

HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

PROJECT OVERVIEW

SHEET

E



POLYMER OVERLAY TRANSITIONAL AREA DETAIL

NOTE: POLYMER OVERLAY TO BE PLACED FULL WIDTH COVERING PAVEMENT AND SHOULDERS.

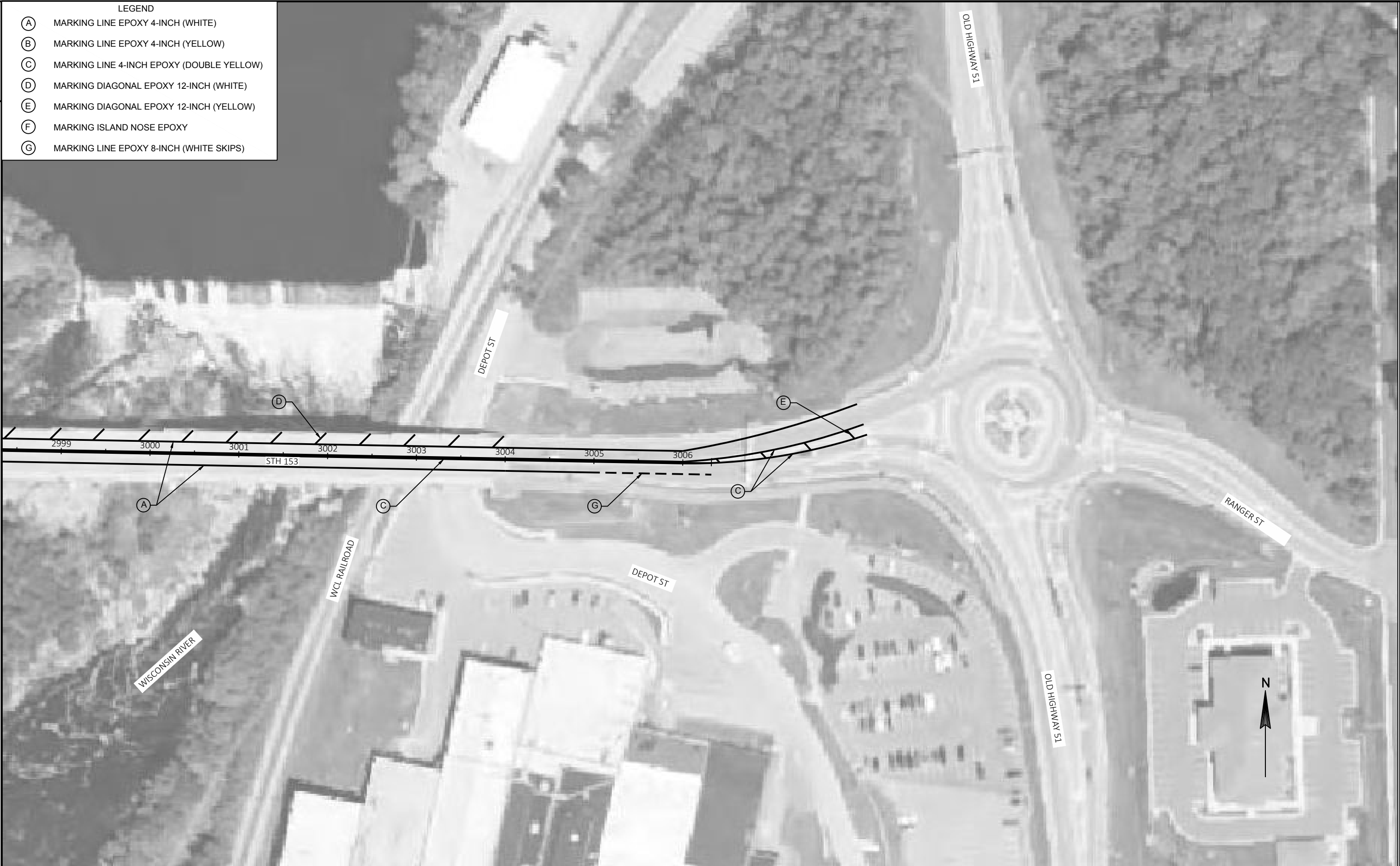
MASK OFF EXISTING STRUCTURE JOINT AS DIRECTED BY THE ENGINEER.

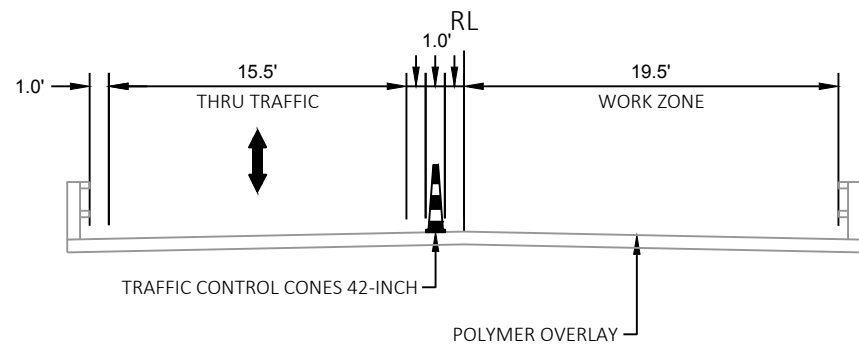
POLYMER TRANSITIONAL AREA INCIDENTAL TO "POLYMER OVERLAY" BID ITEM ON STRUCTURE PLANS.



LEGEND

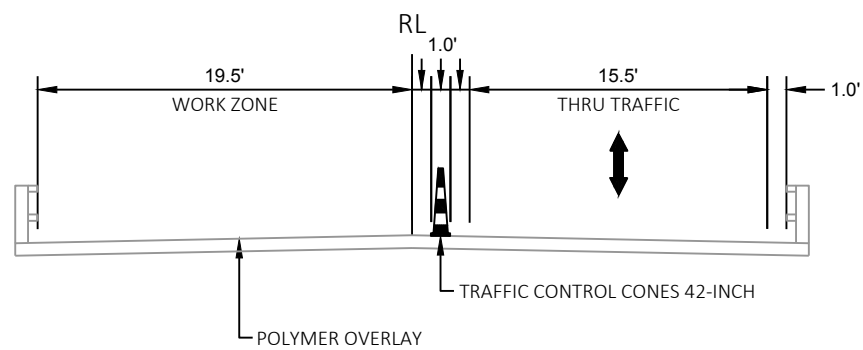
- (A) MARKING LINE EPOXY 4-INCH (WHITE)
- (B) MARKING LINE EPOXY 4-INCH (YELLOW)
- (C) MARKING LINE 4-INCH EPOXY (DOUBLE YELLOW)
- (D) MARKING DIAGONAL EPOXY 12-INCH (WHITE)
- (E) MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
- (F) MARKING ISLAND NOSE EPOXY
- (G) MARKING LINE EPOXY 8-INCH (WHITE SKIPS)





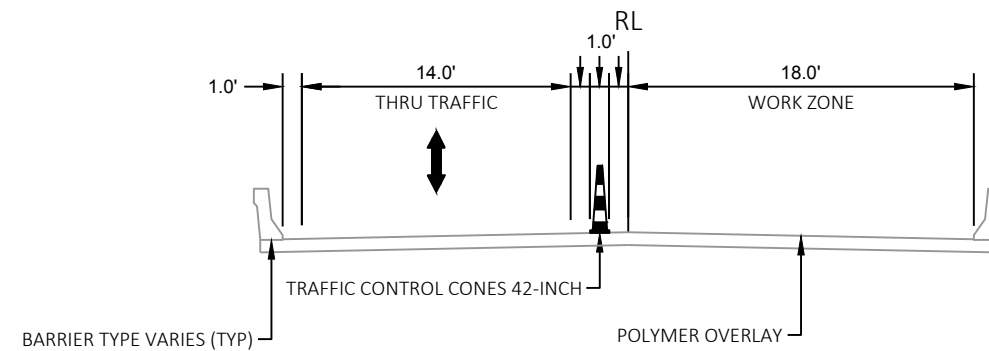
±39' CLEAR WIDTH TYPICAL SECTION - STAGE 1

B-35-101



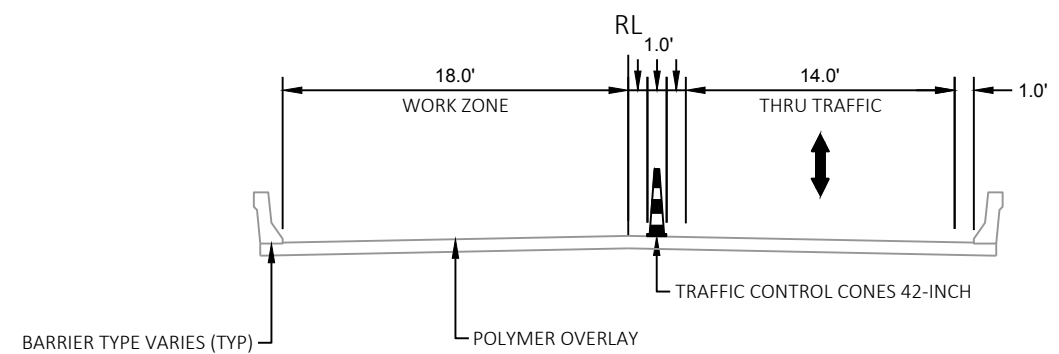
±39' CLEAR WIDTH TYPICAL SECTION - STAGE 2

B-35-101



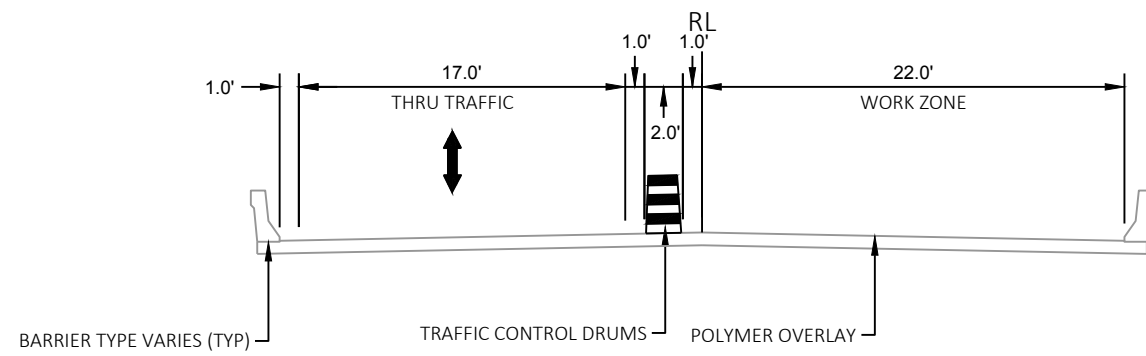
±36' CLEAR WIDTH TYPICAL SECTION - STAGE 1

B-37-305, B-37-318, B-37-319, B-37-320, B-37-330



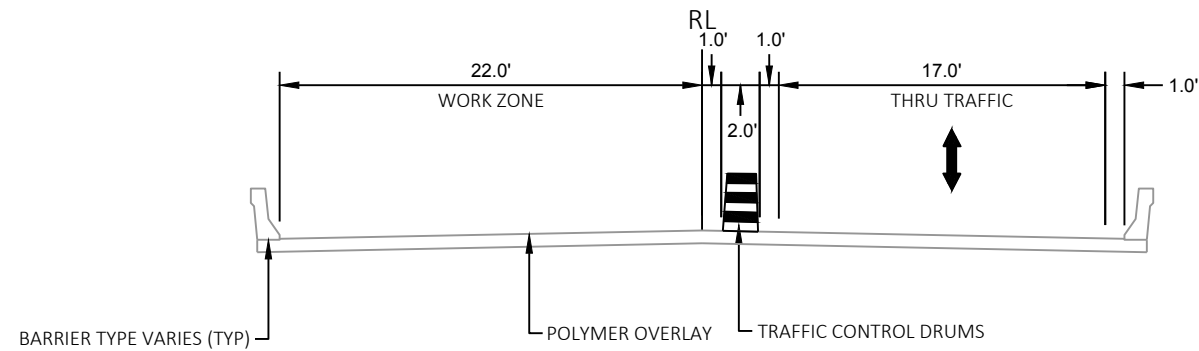
±36' CLEAR WIDTH TYPICAL SECTION - STAGE 2

B-37-305, B-37-318, B-37-319, B-37-320, B-37-330



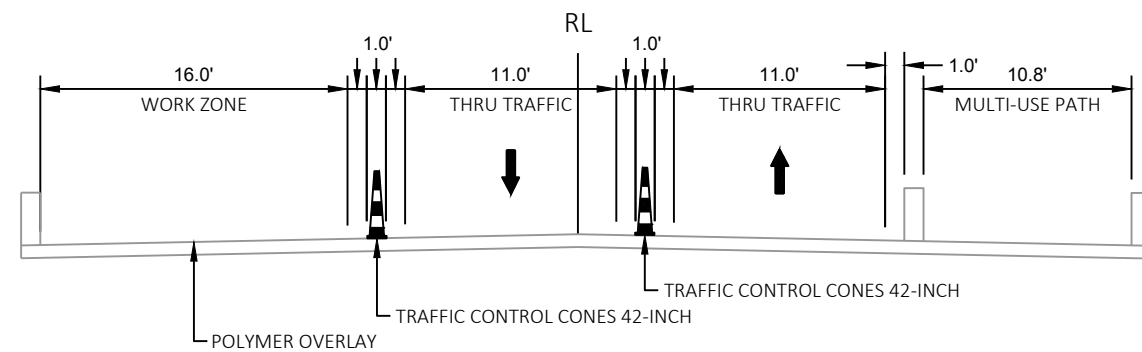
±44' CLEAR WIDTH TYPICAL SECTION - STAGE 1

B-37-6, B-37-306, B-37-308, B-37-315



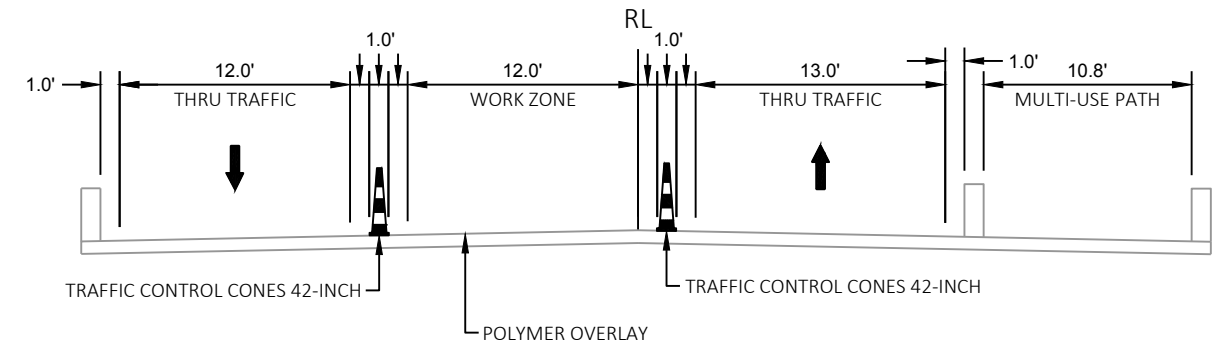
±44' CLEAR WIDTH TYPICAL SECTION - STAGE 2

B-37-6, B-37-306, B-37-308, B-37-315



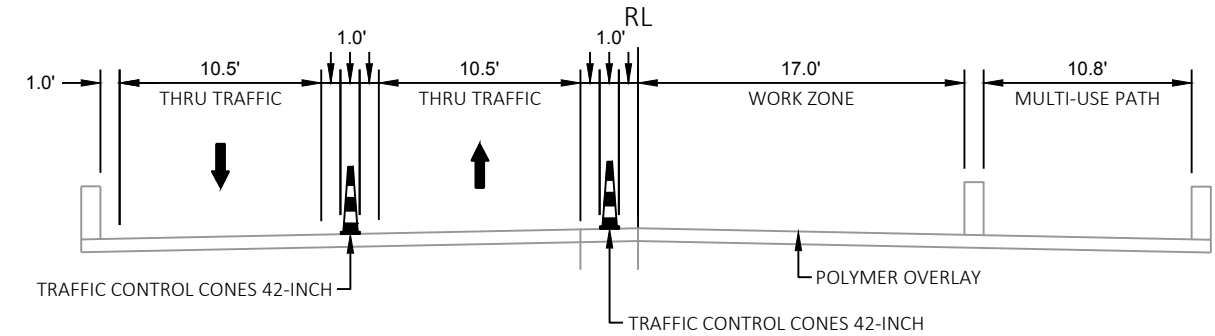
±45' CLEAR WIDTH TYPICAL SECTION - STAGE 1

B-37-89, B-37-96



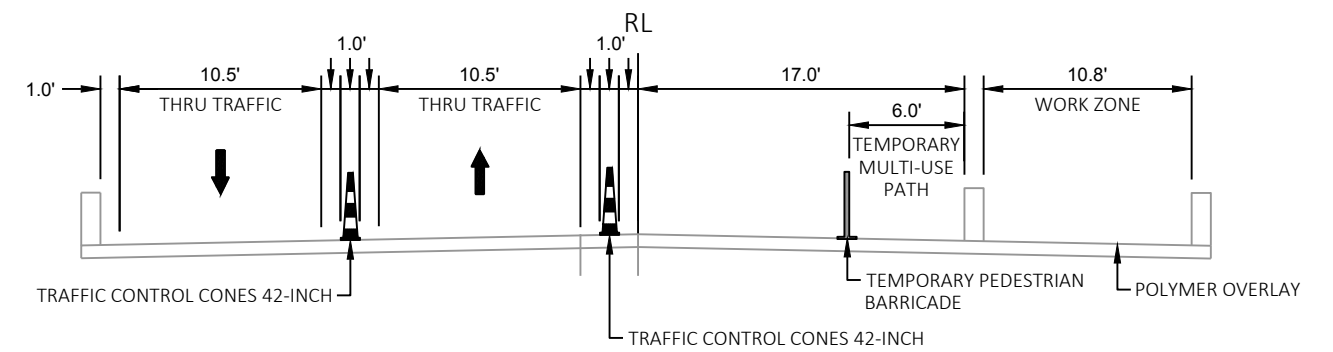
±45' CLEAR WIDTH TYPICAL SECTION - STAGE 2

B-37-89, B-37-96



±45' CLEAR WIDTH TYPICAL SECTION - STAGE 3A

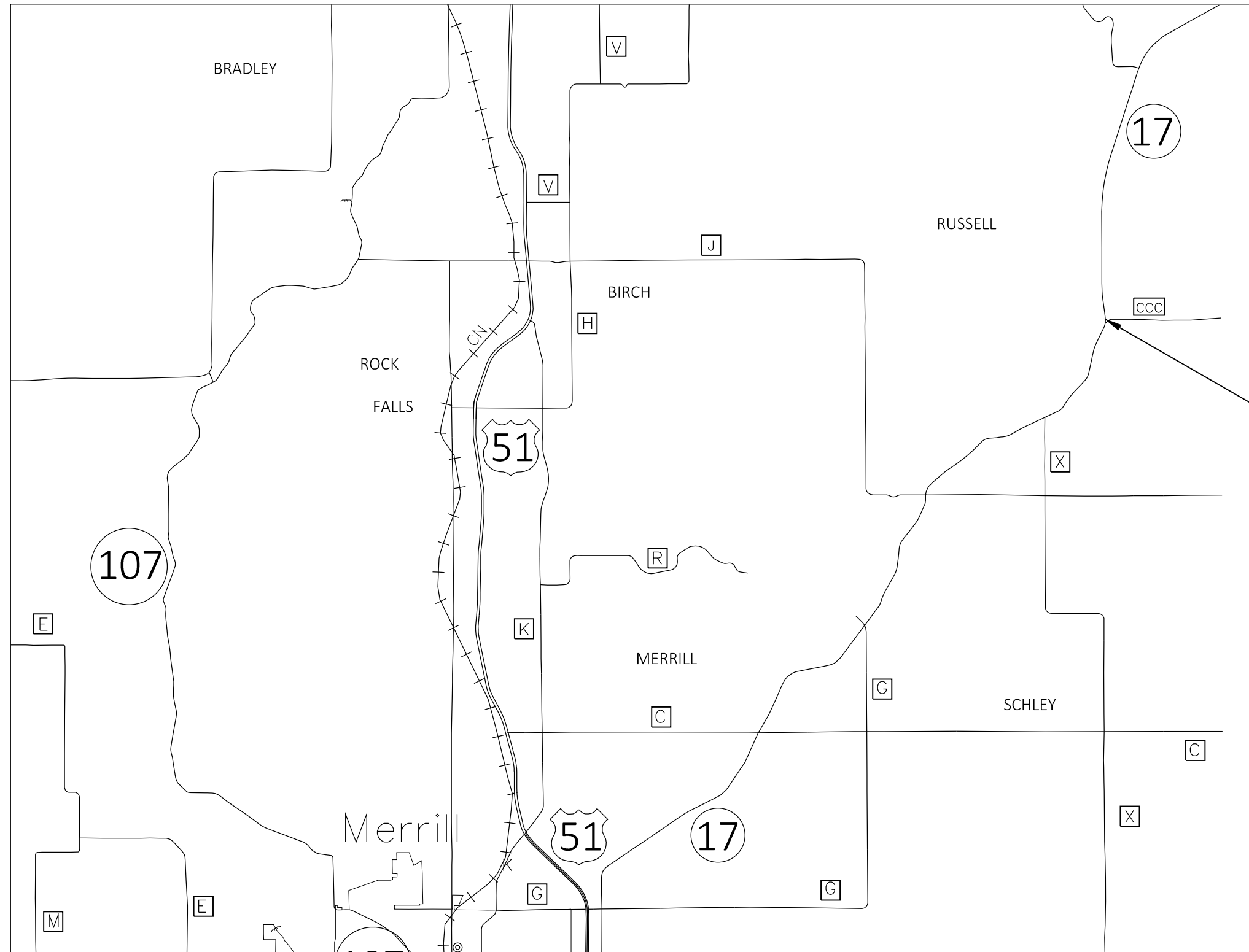
B-37-89, B-37-96



±45' CLEAR WIDTH TYPICAL SECTION - STAGE 3B

B-37-89, B-37-96

PROJECT OVERVIEW - STH 17 BRIDGE



B-35-101
STH 17 OVER THE PRAIRIE RIVER
UTILIZE SDD "TRAFFIC CONTROL, ONE LANE ROAD
STOP CONDITION"

PROJECT NO: 1009-47-61

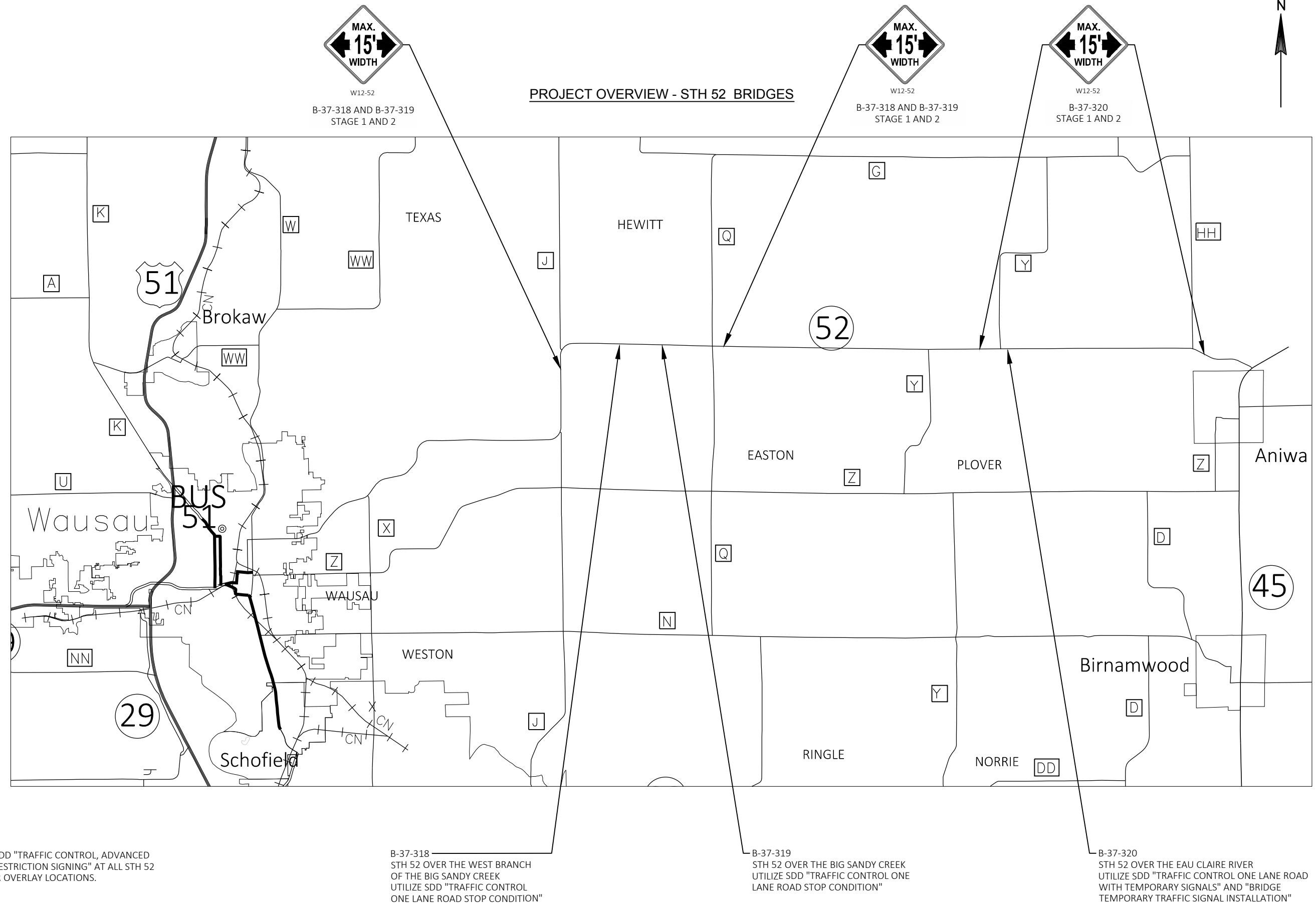
HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

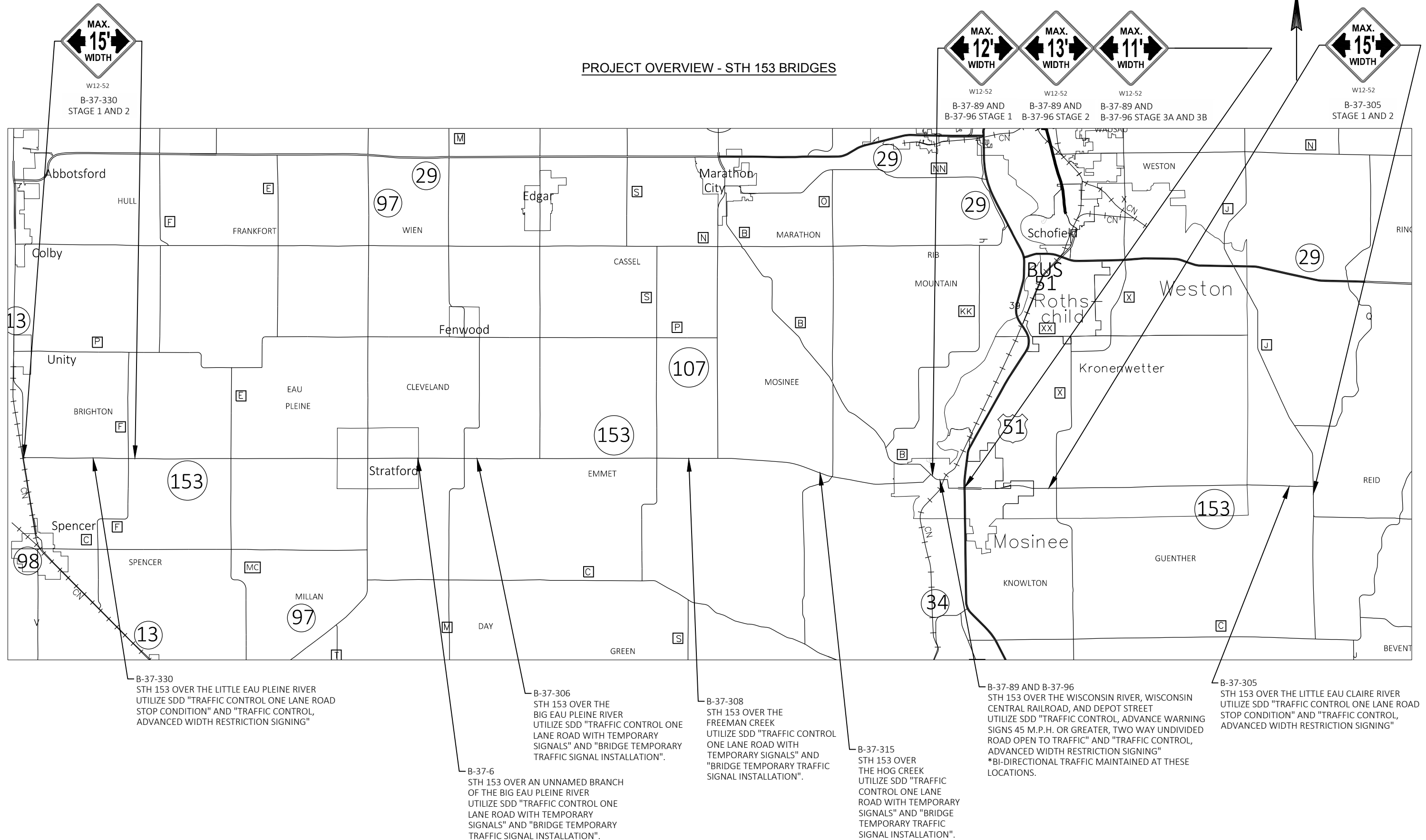
TRAFFIC CONTROL PROJECT OVERVIEW

SHEET

E



PROJECT OVERVIEW - STH 153 BRIDGES



PROJECT NO: 1009-47-61

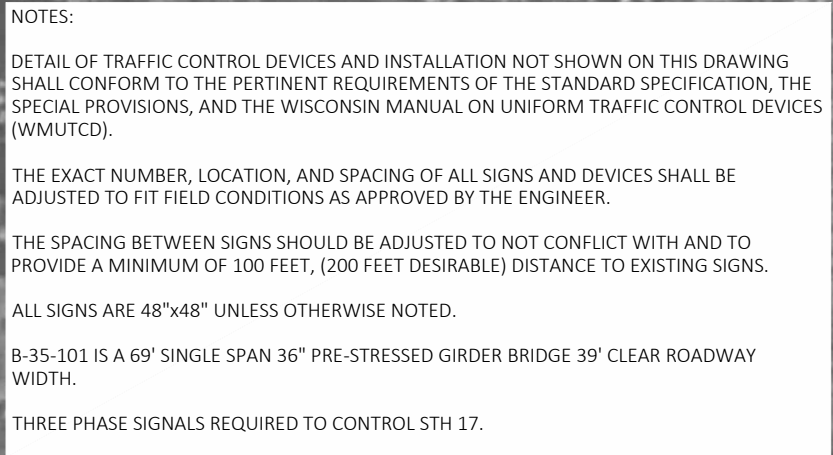
HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE










TRAFFIC CONTROL PROJECT OVERVIEW

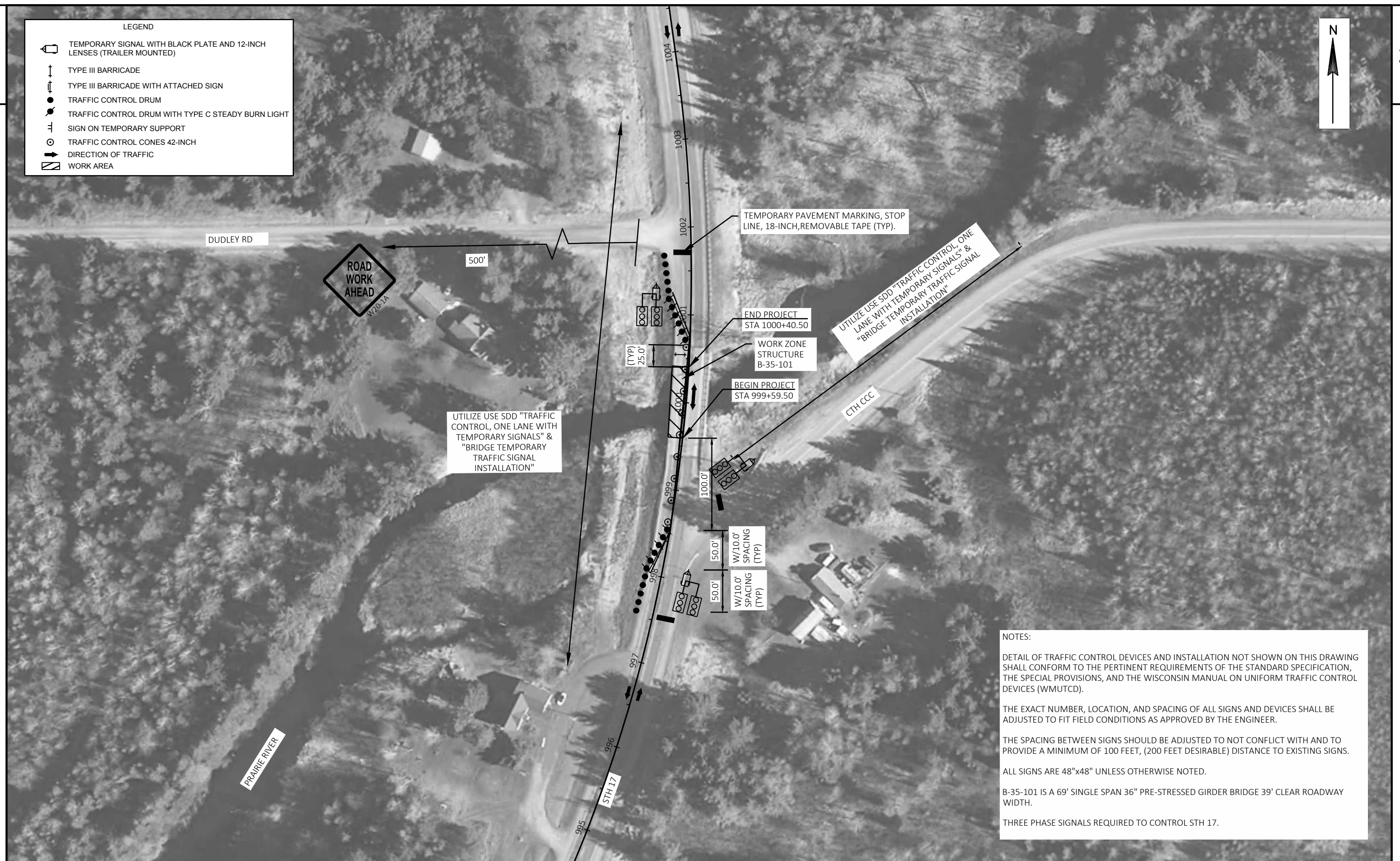
SHEET

E



LEGEND

-  TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL CONES 42-INCH
-  DIRECTION OF TRAFFIC
-  WORK AREA



NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-35-101 IS A 69' SINGLE SPAN 36" PRE-STRESSED GIRDER BRIDGE 39' CLEAR ROADWAY WIDTH.

THREE PHASE SIGNALS REQUIRED TO CONTROL STH 17.

PROJECT NO: 1009-47-61

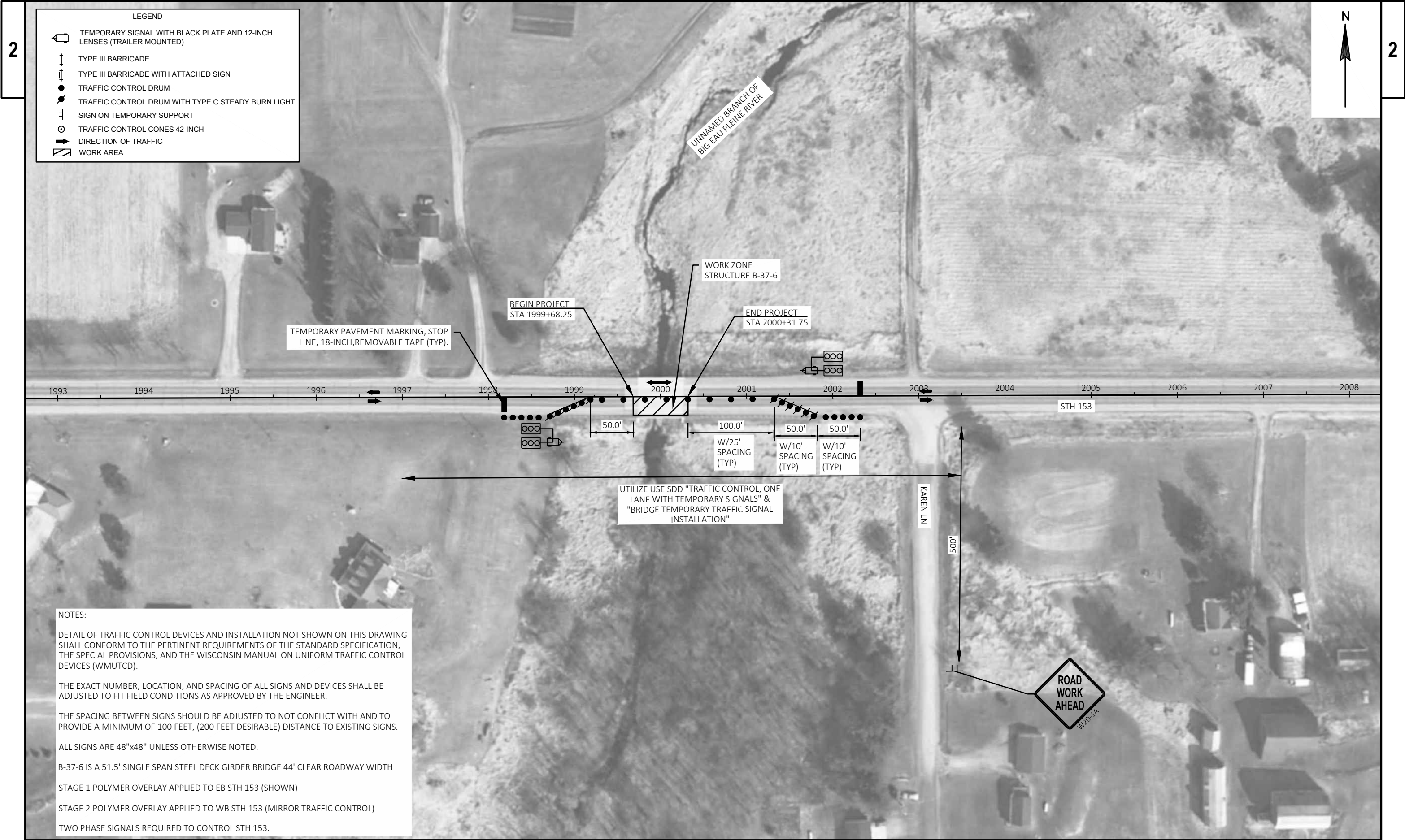
HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 17 (B-35-101-STAGE 2)

SHEET

E



LEGEND

- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL CONES 42-INCH
- DIRECTION OF TRAFFIC
- WORK AREA

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-37-6 IS A 51.5' SINGLE SPAN STEEL DECK GIRDER BRIDGE 44' CLEAR ROADWAY WIDTH

STAGE 1 POLYMER OVERLAY APPLIED TO EB STH 153 (SHOWN)

STAGE 2 POLYMER OVERLAY APPLIED TO WB STH 153 (MIRROR TRAFFIC CONTROL)

TWO PHASE SIGNALS REQUIRED TO CONTROL STH 153.

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

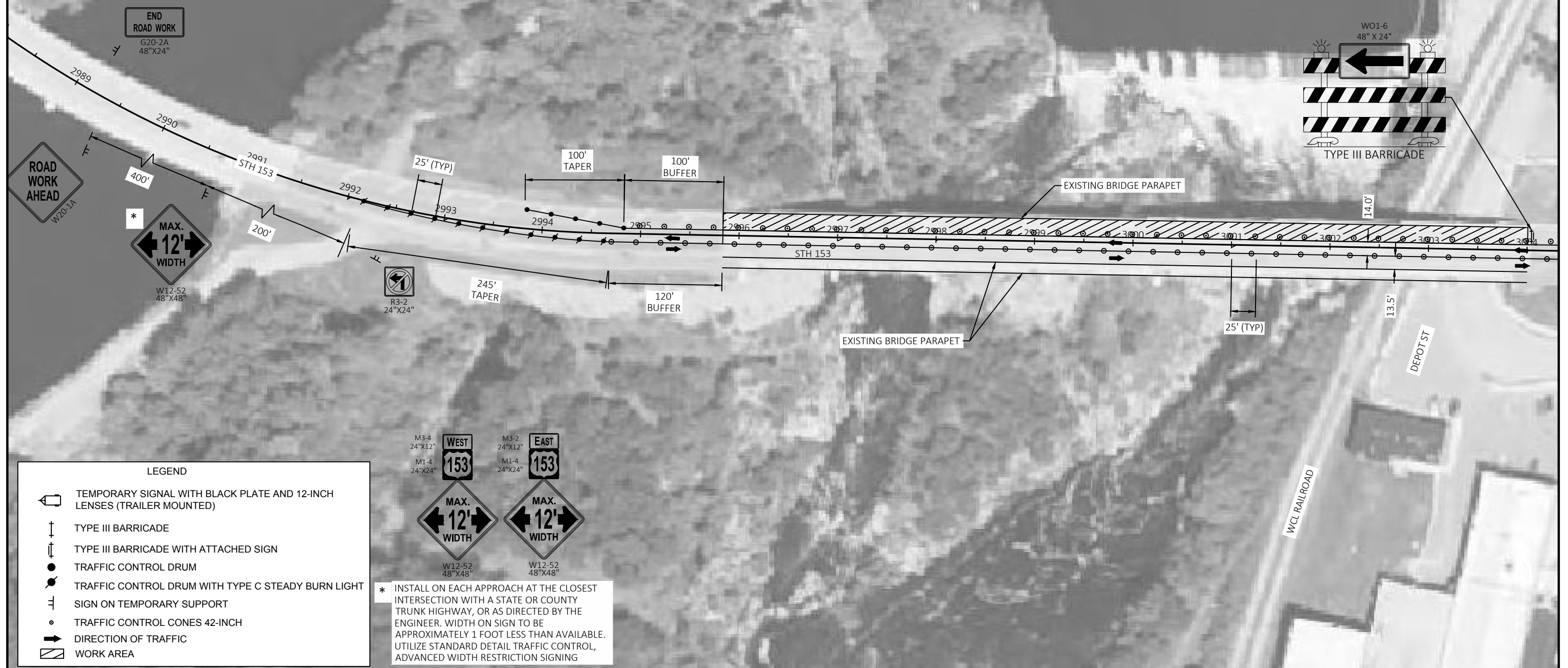
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-89 IS A 779' 8 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH

UTILIZE STANDARD DETAIL DRAWING "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"



NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

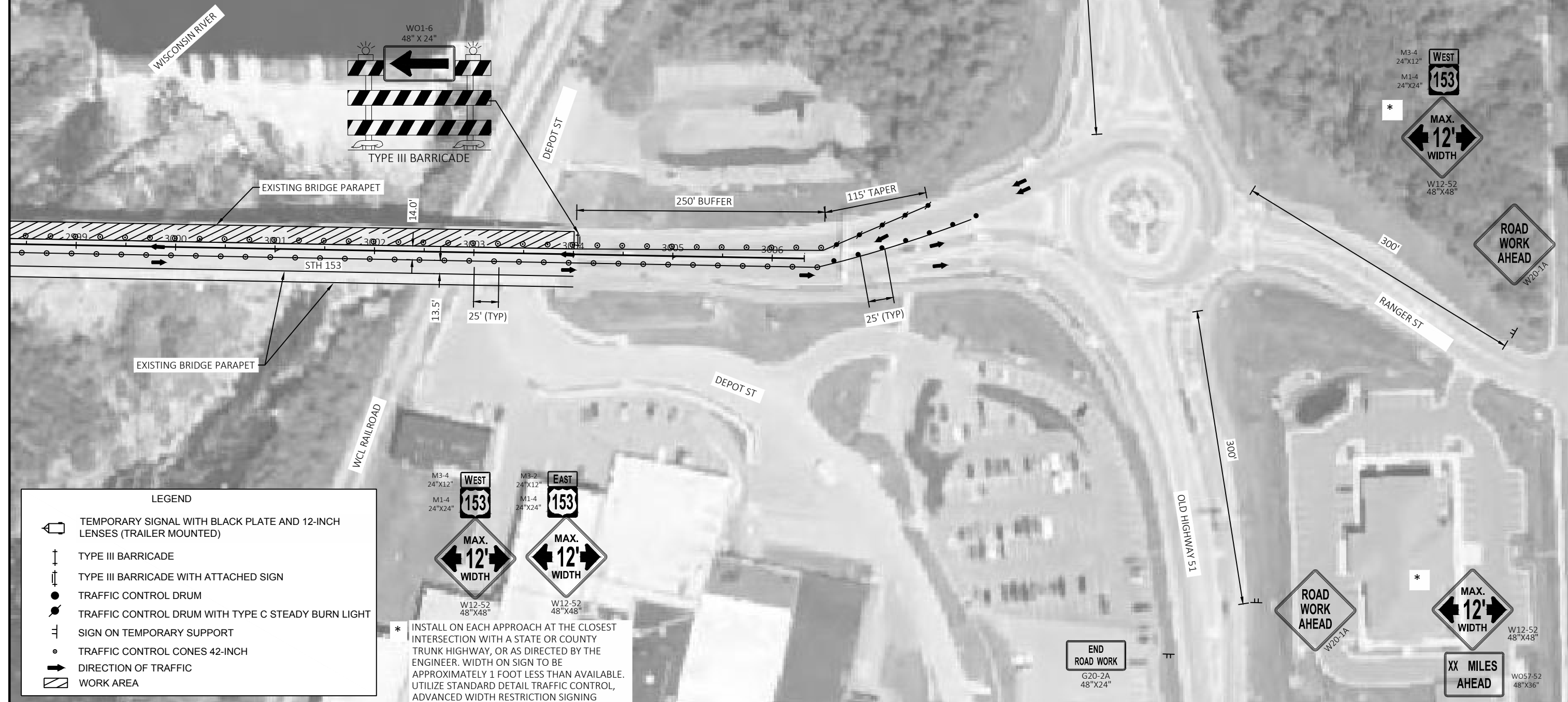
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-89 IS A 779' 8 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH

UTILIZE STANDARD DETAIL DRAWING "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"



PROJECT NO: 1009-47-61

HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 153 (B-37-89) STAGE 1

SHEET

E

NOTES:

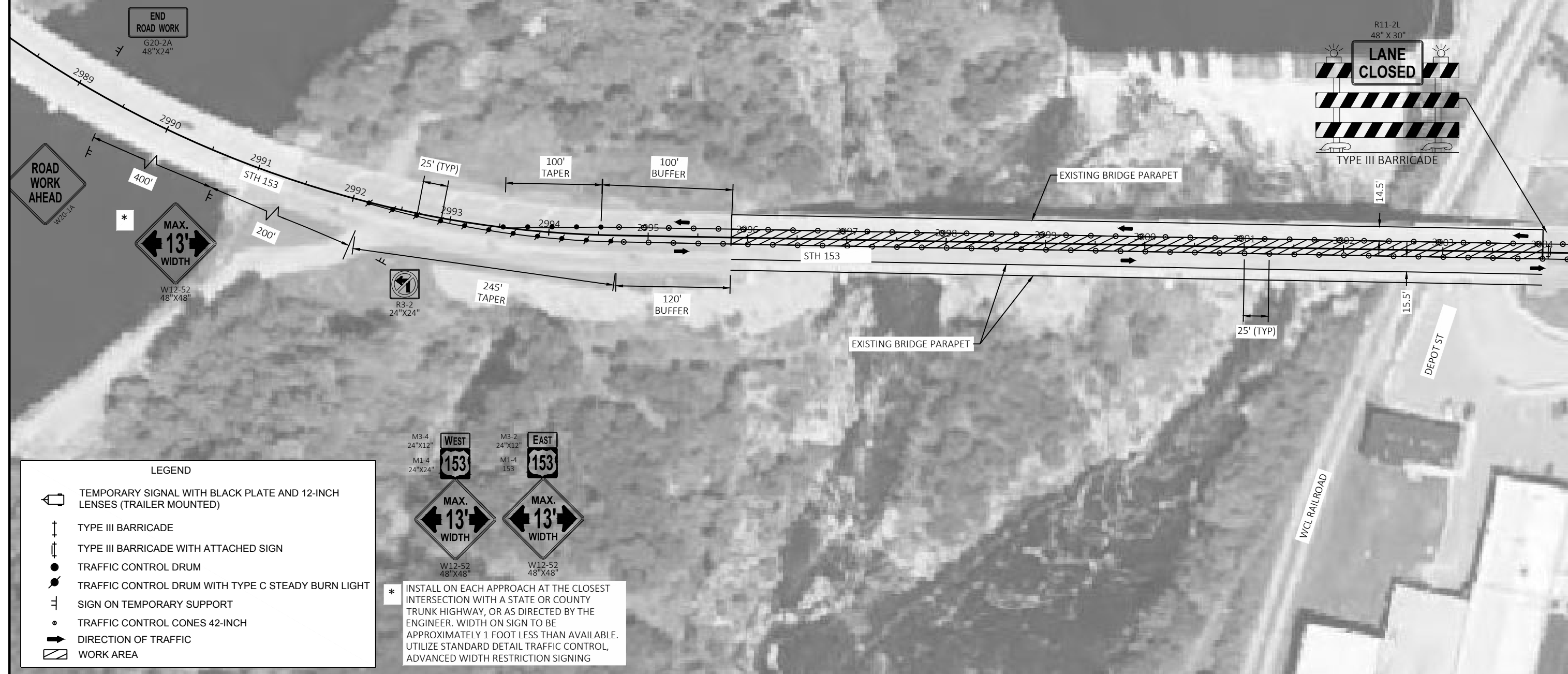
DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-89 IS A 779' 8 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH



LEGEND

- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL CONES 42-INCH
- DIRECTION OF TRAFFIC
- WORK AREA

* INSTALL ON EACH APPROACH AT THE CLOSEST INTERSECTION WITH A STATE OR COUNTY TRUNK HIGHWAY, OR AS DIRECTED BY THE ENGINEER. WIDTH ON SIGN TO BE APPROXIMATELY 1 FOOT LESS THAN AVAILABLE. UTILIZE STANDARD DETAIL TRAFFIC CONTROL, ADVANCED WIDTH RESTRICTION SIGNING

PROJECT NO: 1009-47-61

HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 153 (B-37-89) STAGE 2

SHEET

E

NOTES:

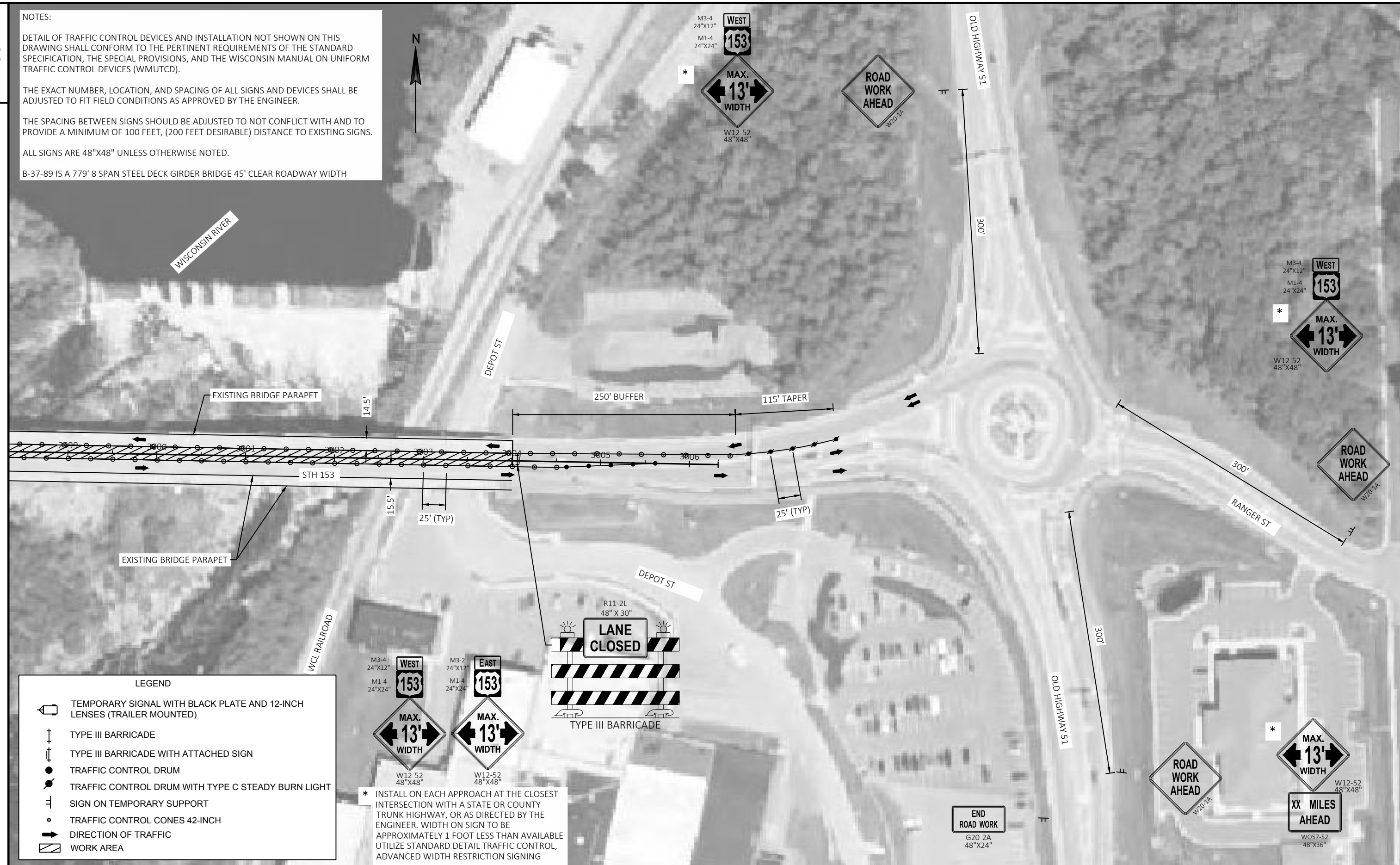
DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-89 IS A 779' 8 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH



PROJECT NO: 1009-47-61

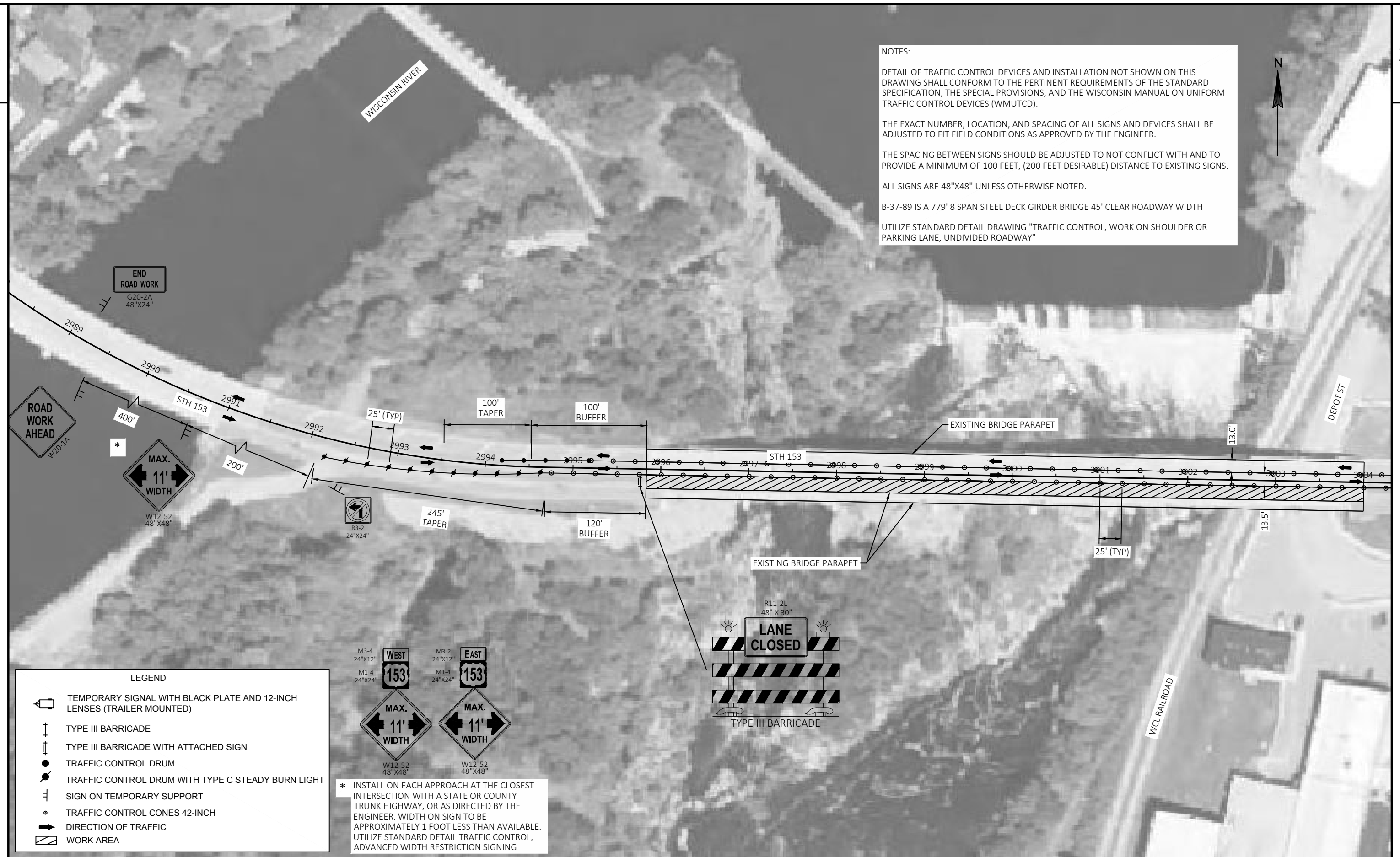
HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 153 (B-37-89) STAGE 2

SHEET

E



PROJECT NO: 1009-47-61

HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 153 (B-37-89) STAGE 3A

SHEET

E

FILE NAME : P:\UZ\W\WITNC\145683\CIVIL 3D\SHEETS\PLAN\B-37-89 STAGE 3A.DWG
LAYOUT NAME - B-37-89 Stage 3A (1)

PLOT DATE : 1/25/2019 2:51 PM

PLOT BY : NICK ENGH

PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADDs SHEET 42

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

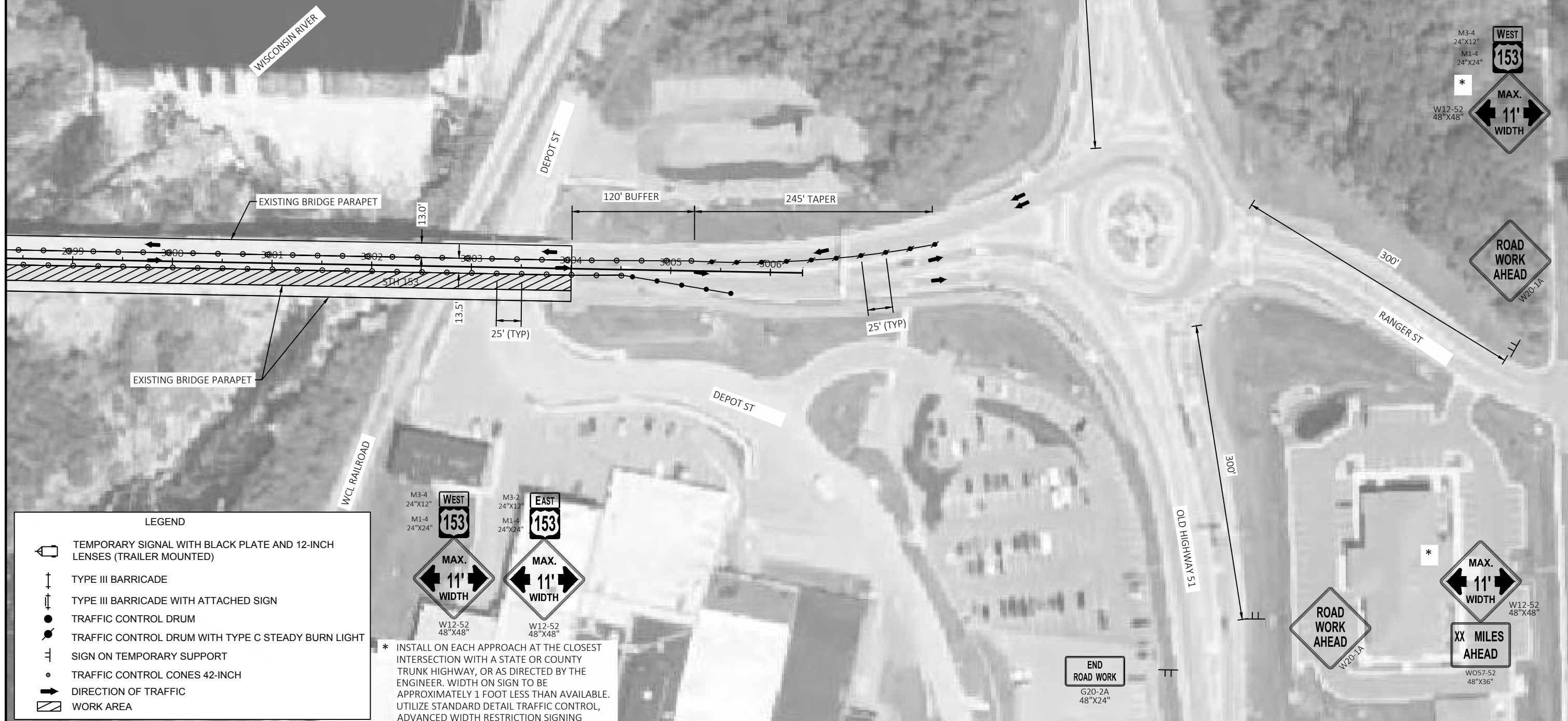
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

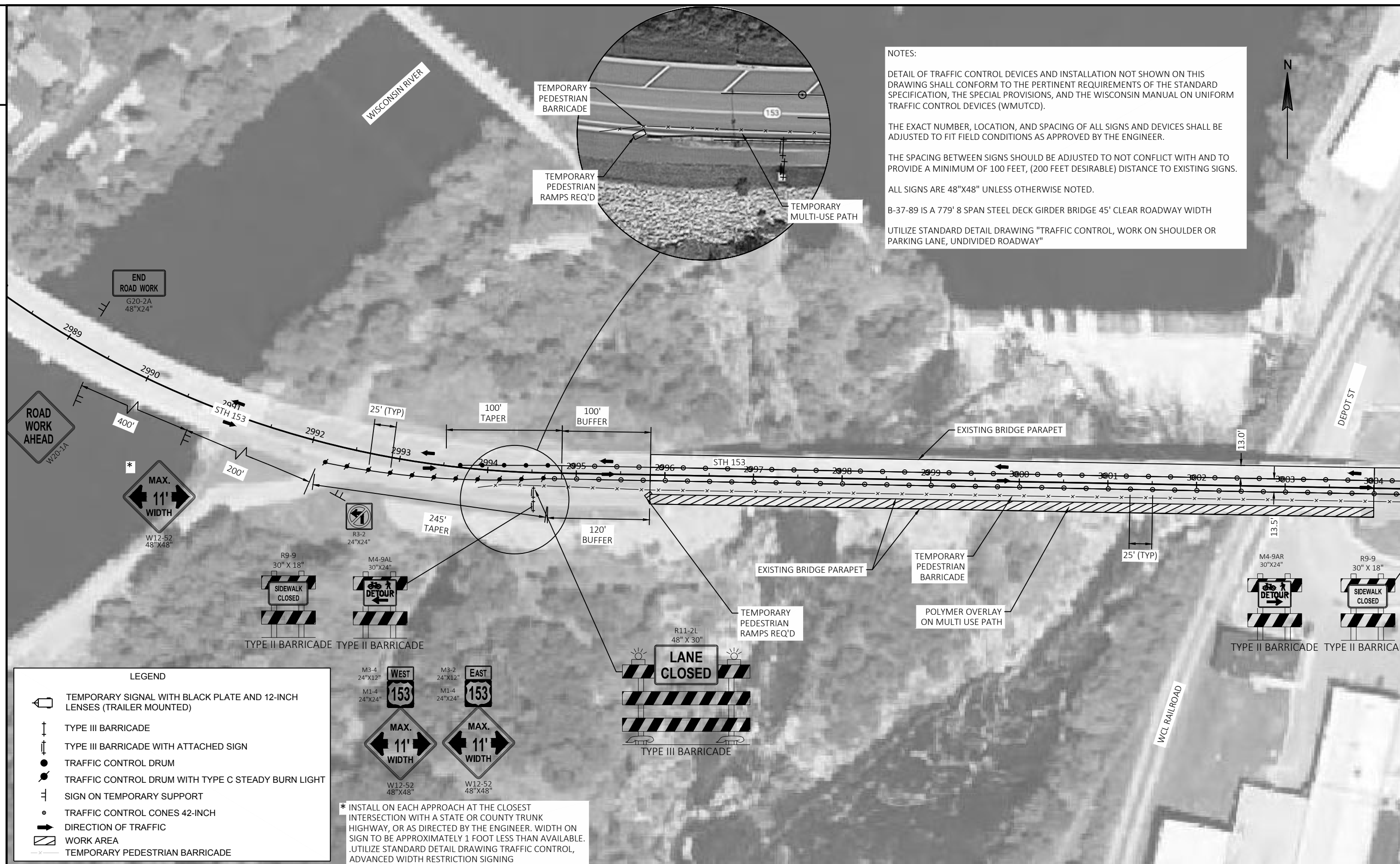
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-89 IS A 779' 8 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH

UTILIZE STANDARD DETAIL DRAWING "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"





PROJECT NO: 1009-47-61

HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 153 (B-37-89) STAGE 3B

SHEET

E

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

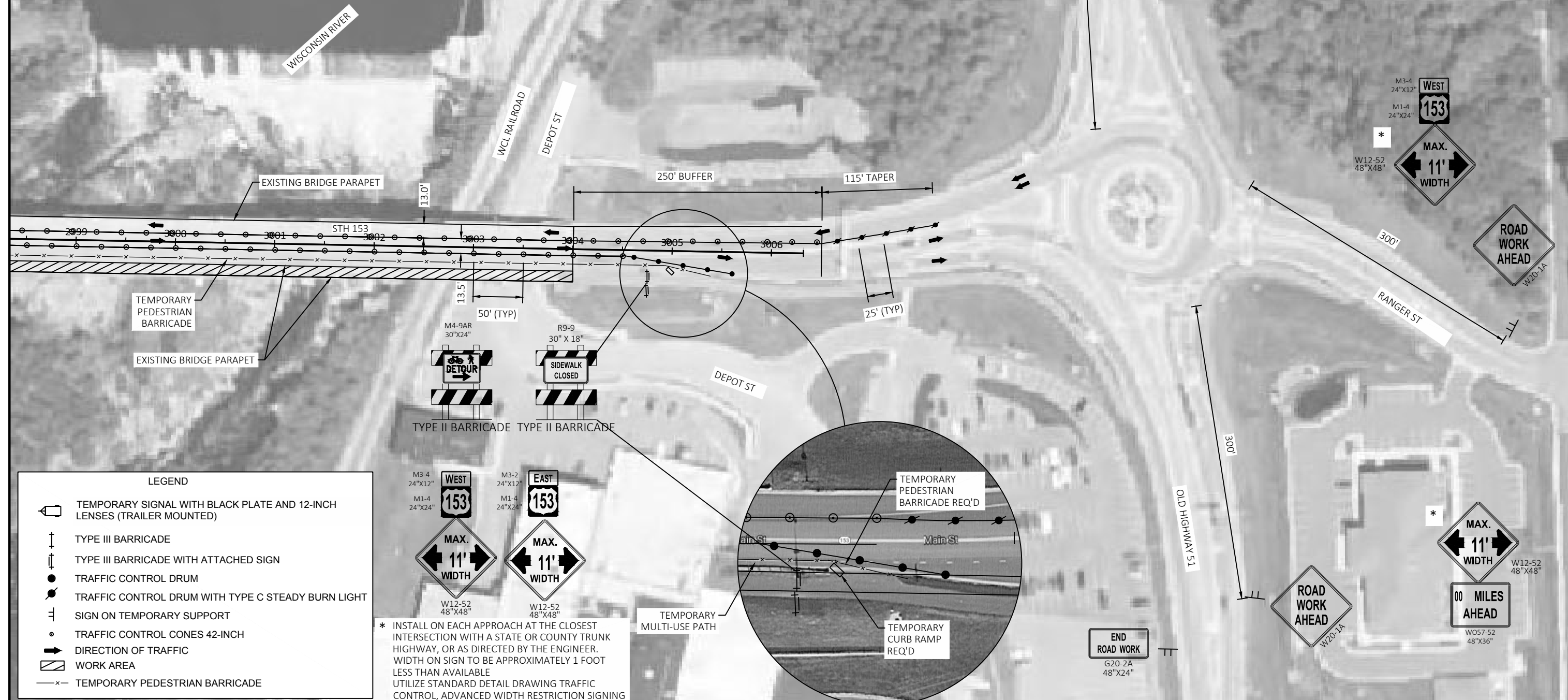
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-89 IS A 779' 8 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH

UTILIZE STANDARD DETAIL DRAWING "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"



PROJECT NO: 1009-47-61

HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 153 (B-37-89) STAGE 3B

SHEET

E

FILE NAME : P:\U2\W\WITNC\145683\CIVIL 3D\SHEETSPLAN\B-37-89 STAGE 3B.DWG
LAYOUT NAME - B-37-89 Stage 3B (2)

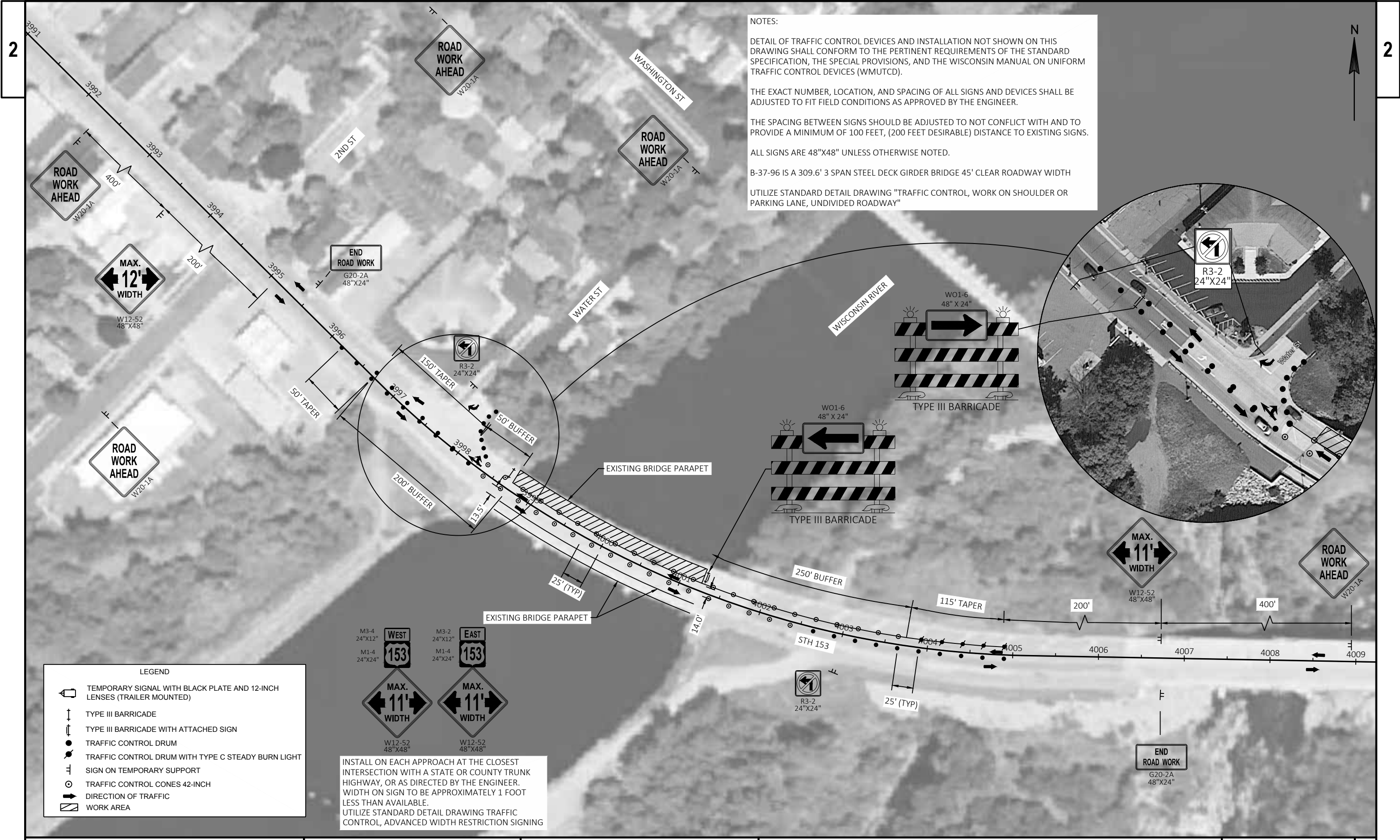
PLOT DATE : 1/25/2019 2:51 PM

PLOT BY : NICK ENGH

PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADDs SHEET 42



NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

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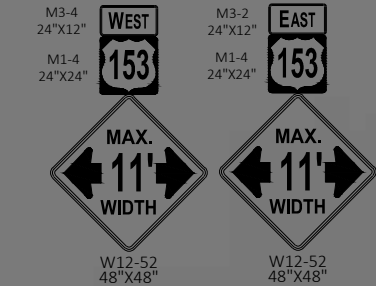
ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-96 IS A 309.6' 3 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH

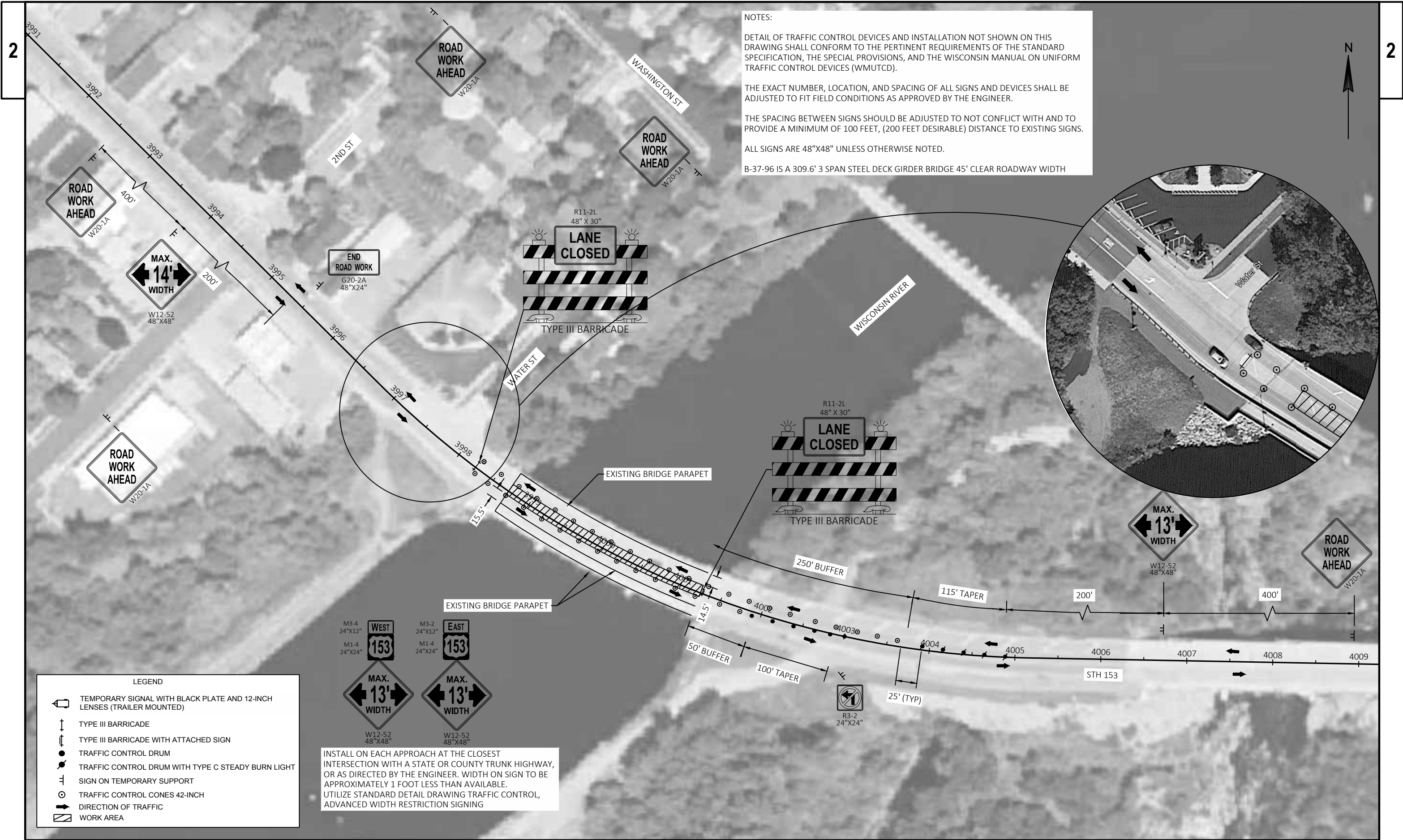
UTILIZE STANDARD DETAIL DRAWING "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"

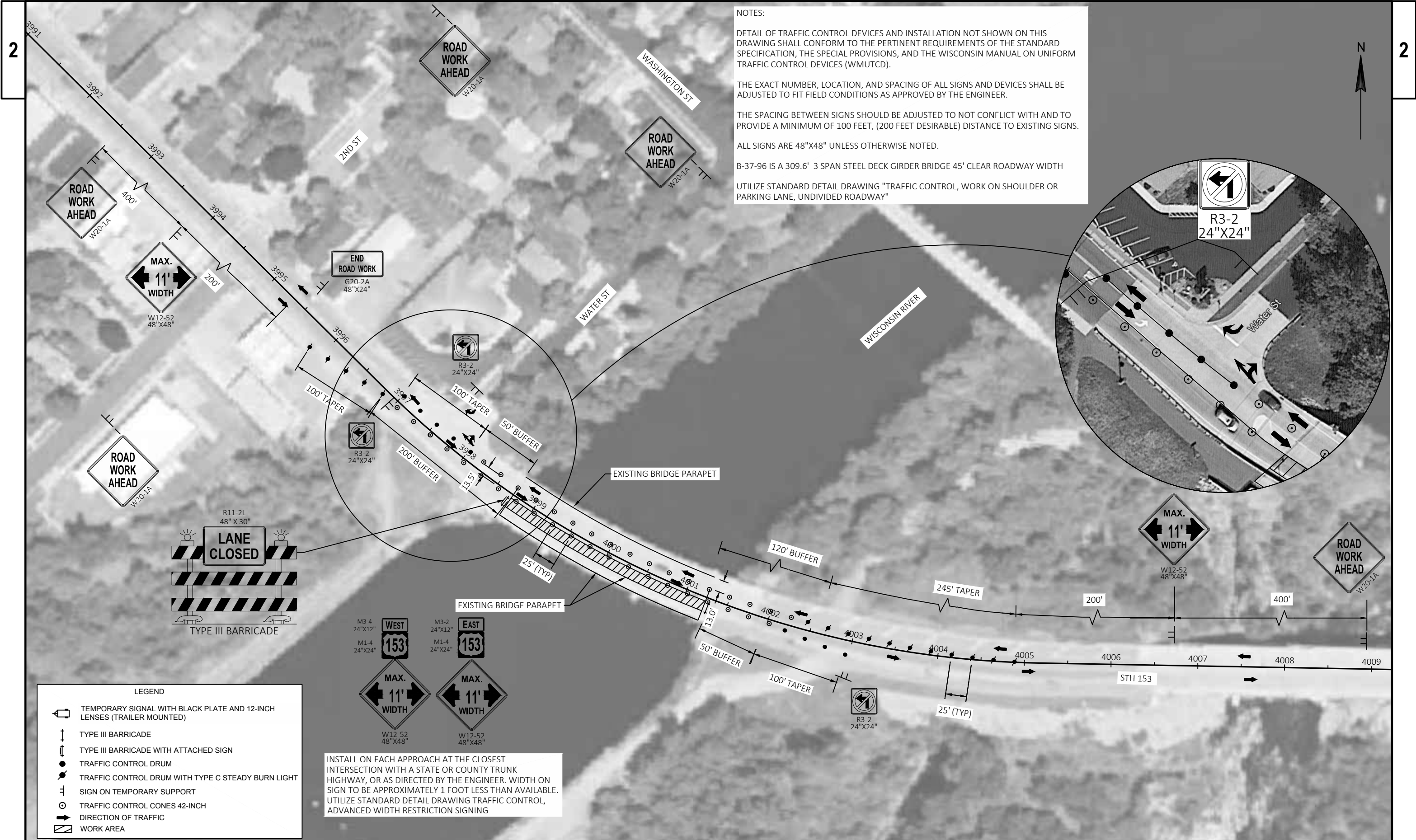


- LEGEND
- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TRAFFIC CONTROL DRUM
 - TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
 - SIGN ON TEMPORARY SUPPORT
 - TRAFFIC CONTROL CONES 42-INCH
 - DIRECTION OF TRAFFIC
 - WORK AREA



INSTALL ON EACH APPROACH AT THE CLOSEST INTERSECTION WITH A STATE OR COUNTY TRUNK HIGHWAY, OR AS DIRECTED BY THE ENGINEER. WIDTH ON SIGN TO BE APPROXIMATELY 1 FOOT LESS THAN AVAILABLE. UTILIZE STANDARD DETAIL DRAWING TRAFFIC CONTROL, ADVANCED WIDTH RESTRICTION SIGNING





NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

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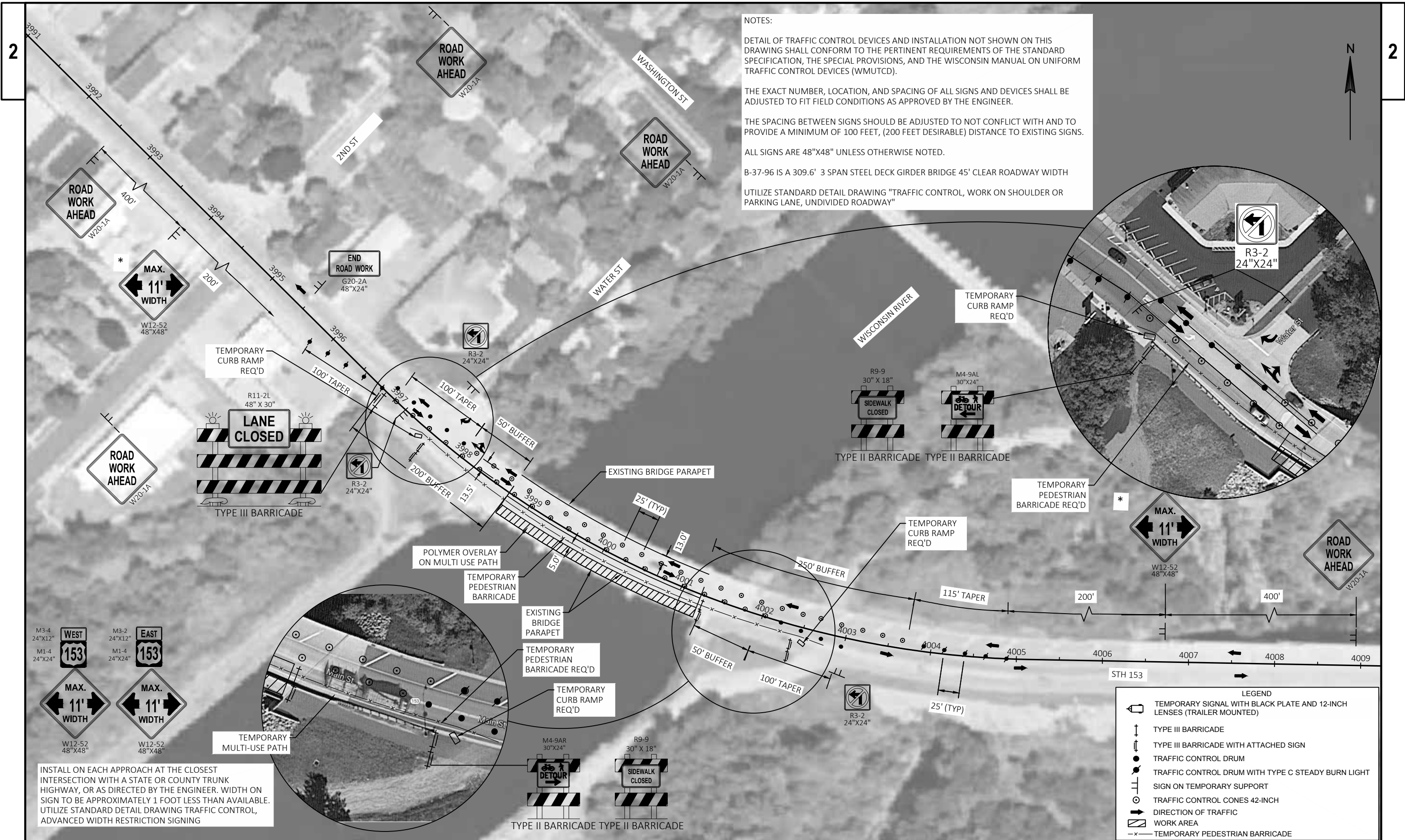
ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

B-37-96 IS A 309.6' 3 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH

UTILIZE STANDARD DETAIL DRAWING "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"

- LEGEND
- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TRAFFIC CONTROL DRUM
 - TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
 - SIGN ON TEMPORARY SUPPORT
 - TRAFFIC CONTROL CONES 42-INCH
 - DIRECTION OF TRAFFIC
 - WORK AREA

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DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

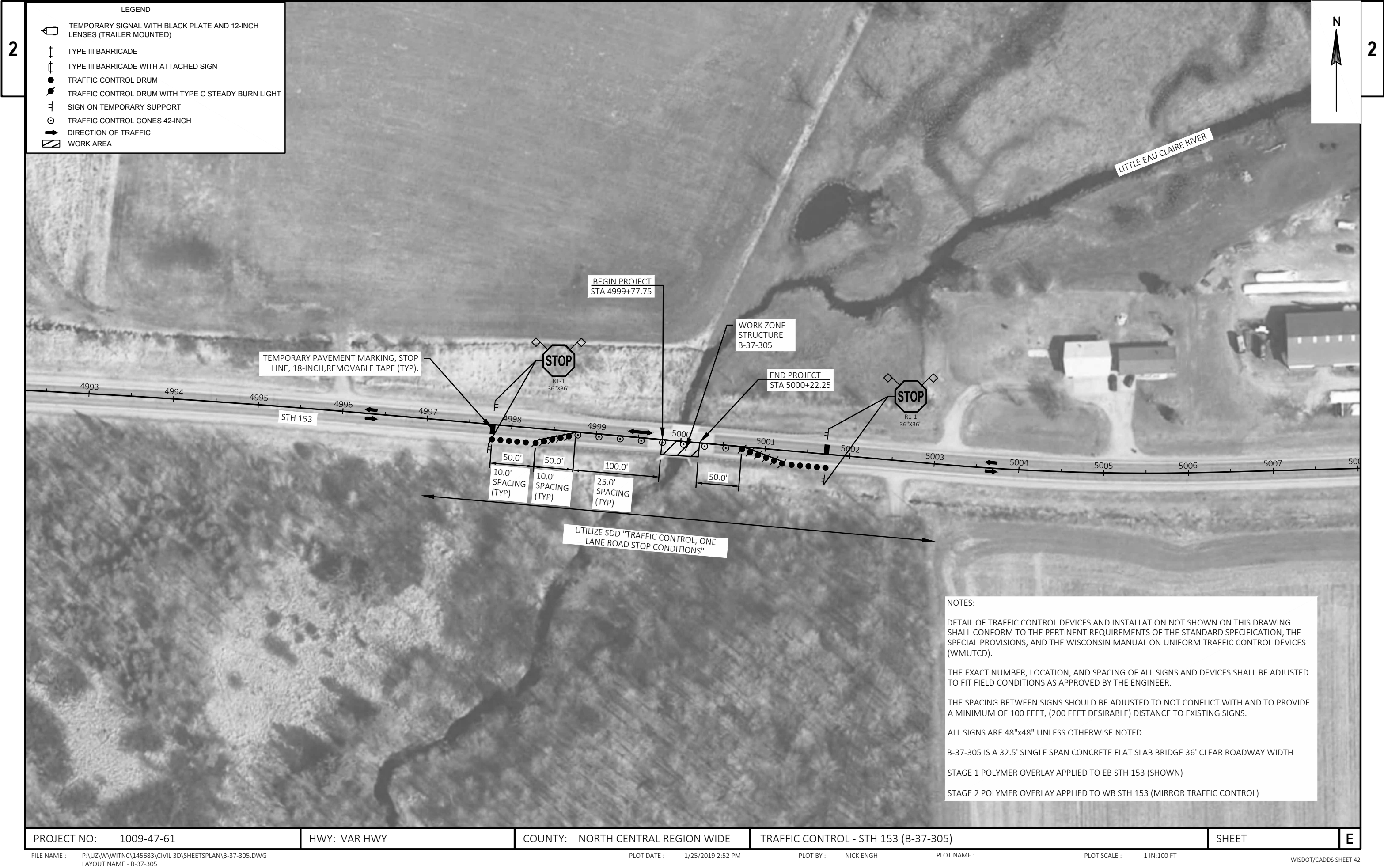
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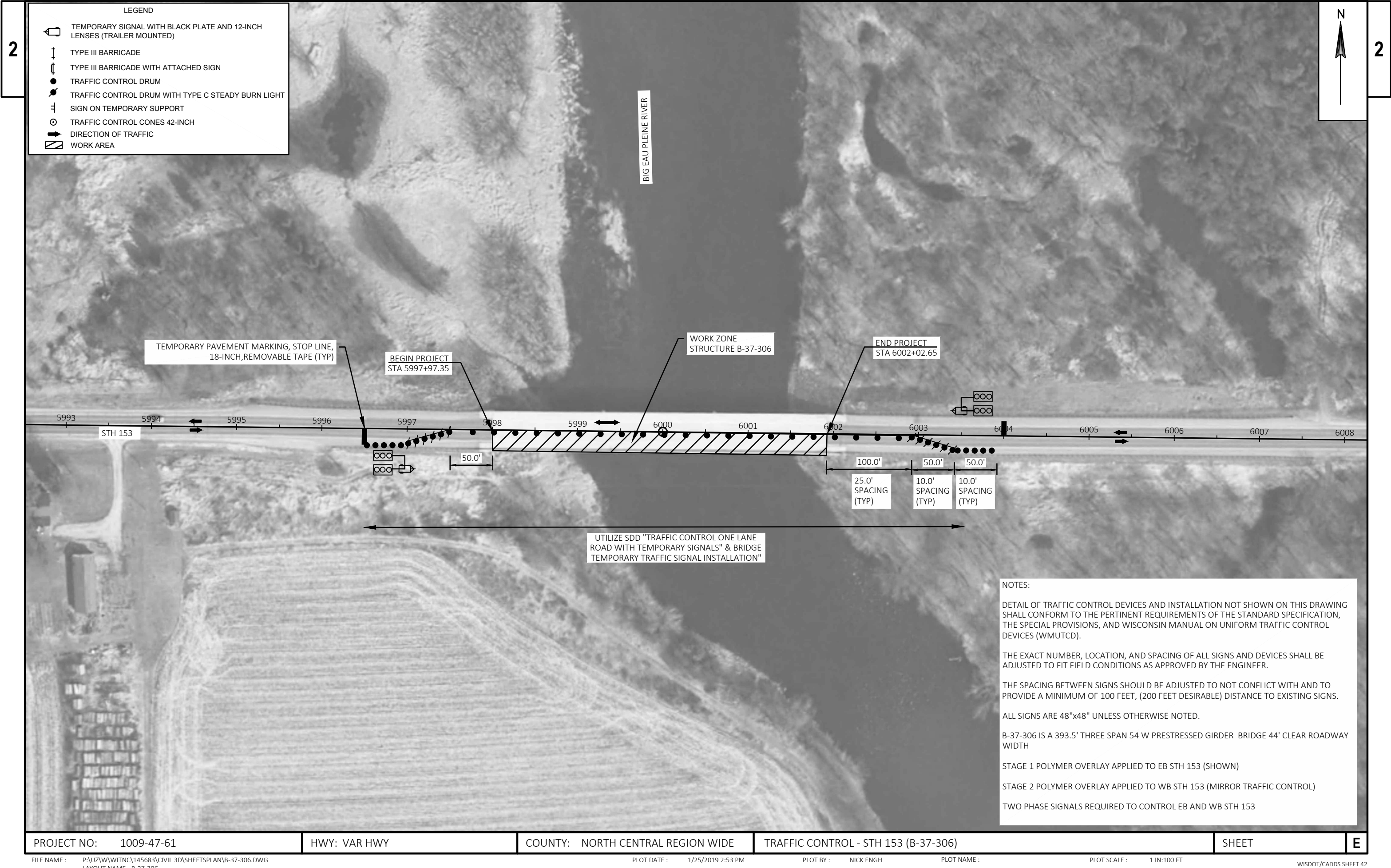
ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

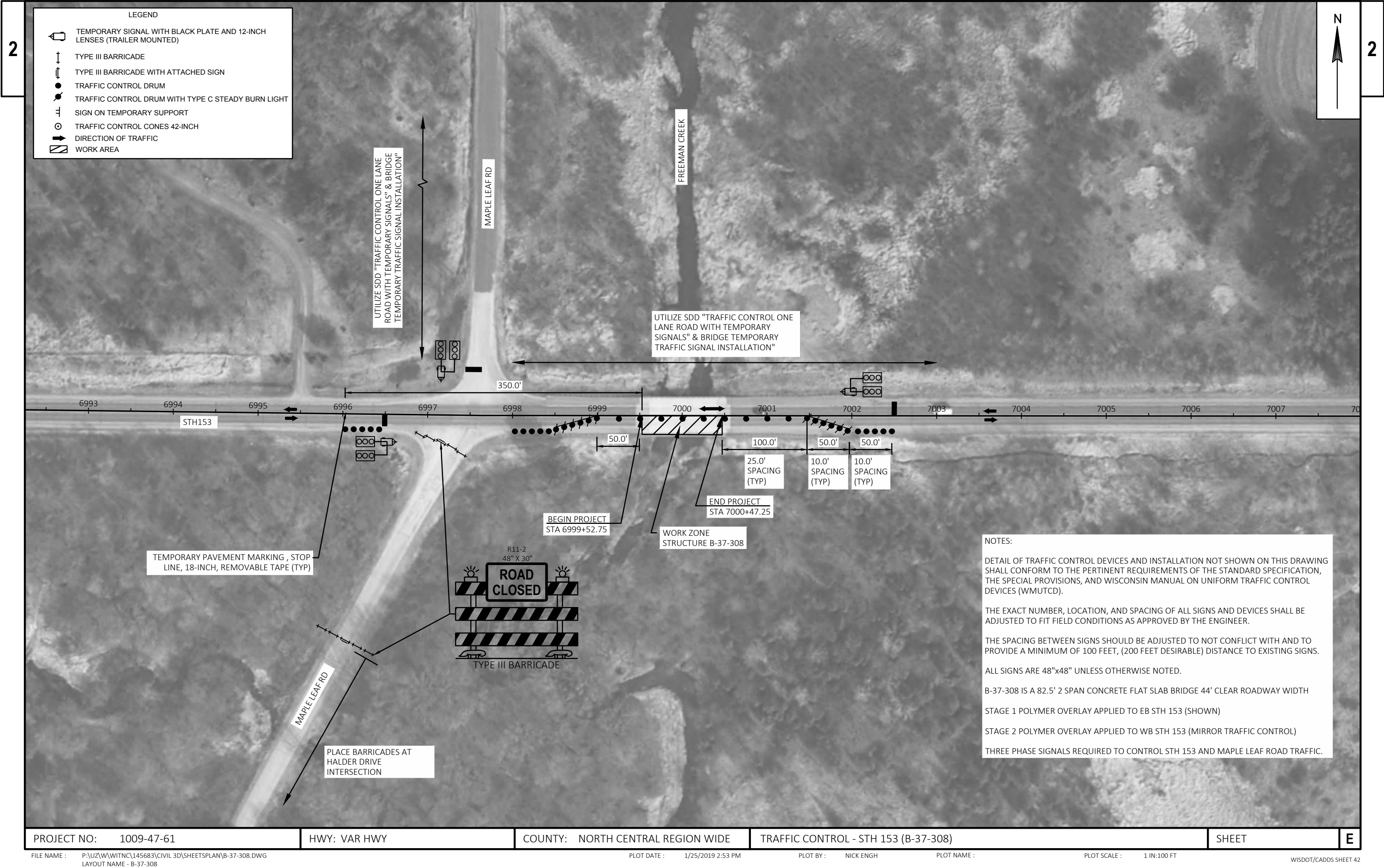
B-37-96 IS A 309.6' 3 SPAN STEEL DECK GIRDER BRIDGE 45' CLEAR ROADWAY WIDTH

UTILIZE STANDARD DETAIL DRAWING "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"










INSTALL ON EACH APPROACH AT THE CLOSEST INTERSECTION WITH A STATE OR COUNTY TRUNK HIGHWAY, OR AS DIRECTED BY THE ENGINEER. WIDTH ON SIGN TO BE APPROXIMATELY 1 FOOT LESS THAN AVAILABLE. UTILIZE STANDARD DETAIL DRAWING TRAFFIC CONTROL, ADVANCED WIDTH RESTRICTION SIGNING

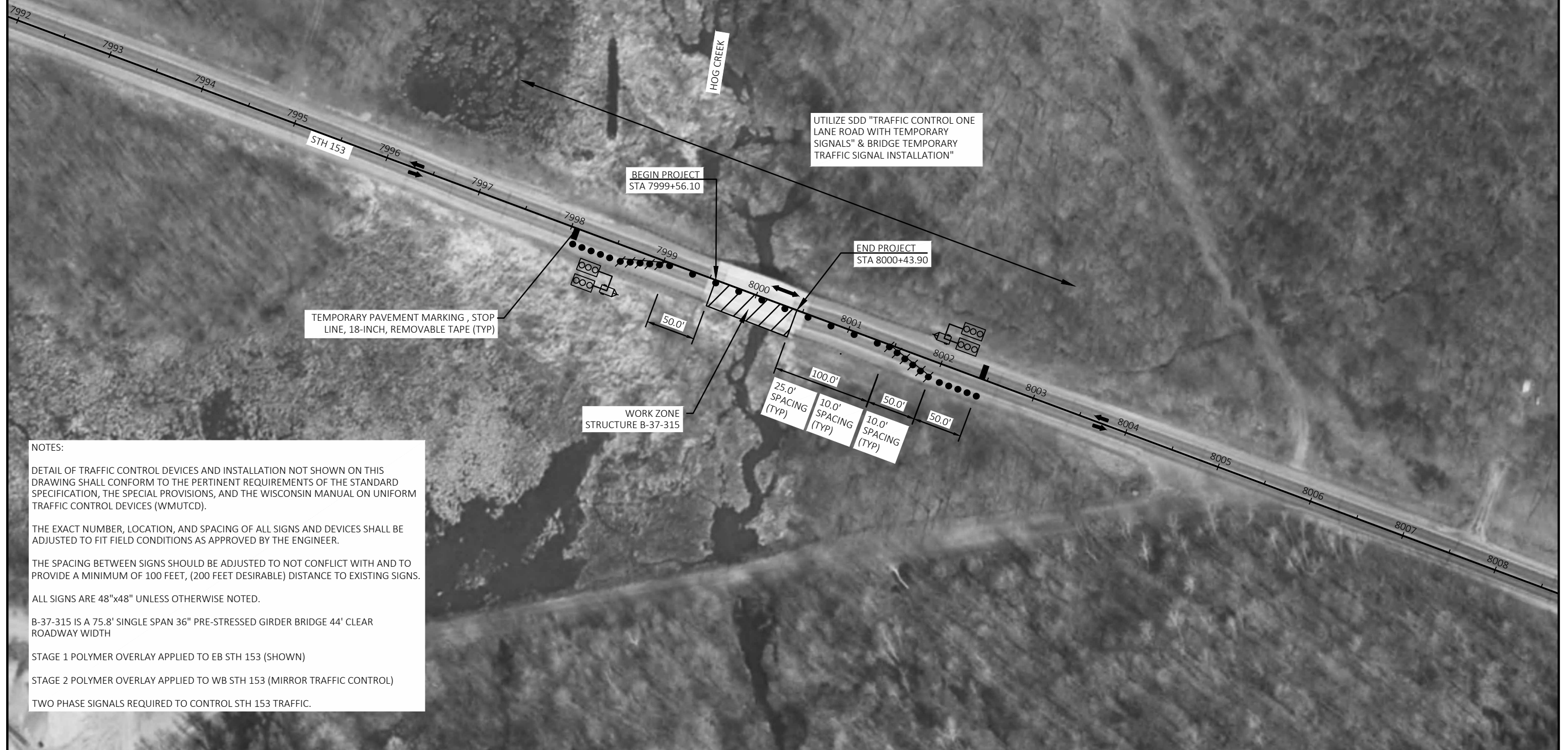






LEGEND

-  TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL CONES 42-INCH
-  DIRECTION OF TRAFFIC
-  WORK AREA



PROJECT NO: 1009-47-61

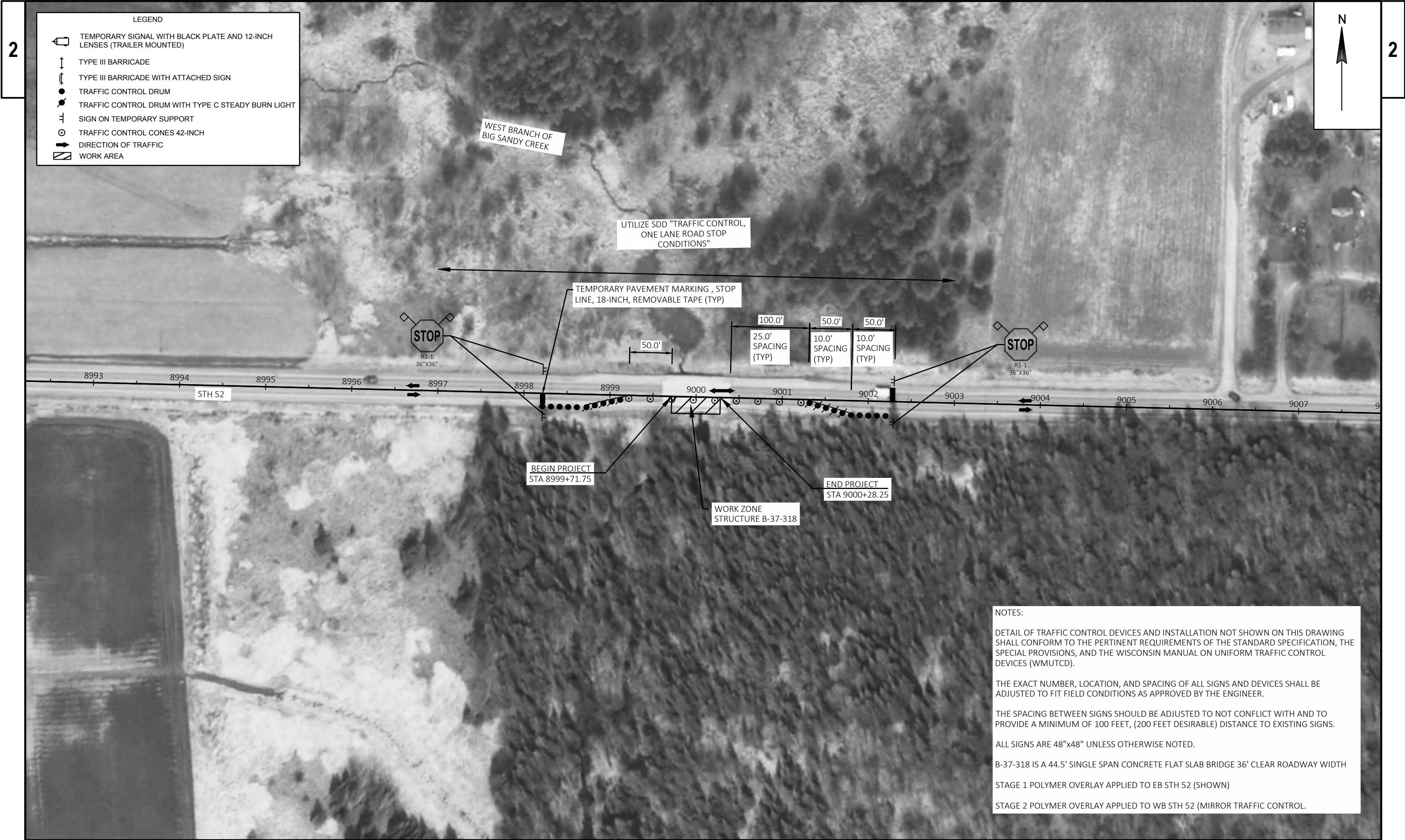
HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 153 (B-37-315)

SHEET

E



NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

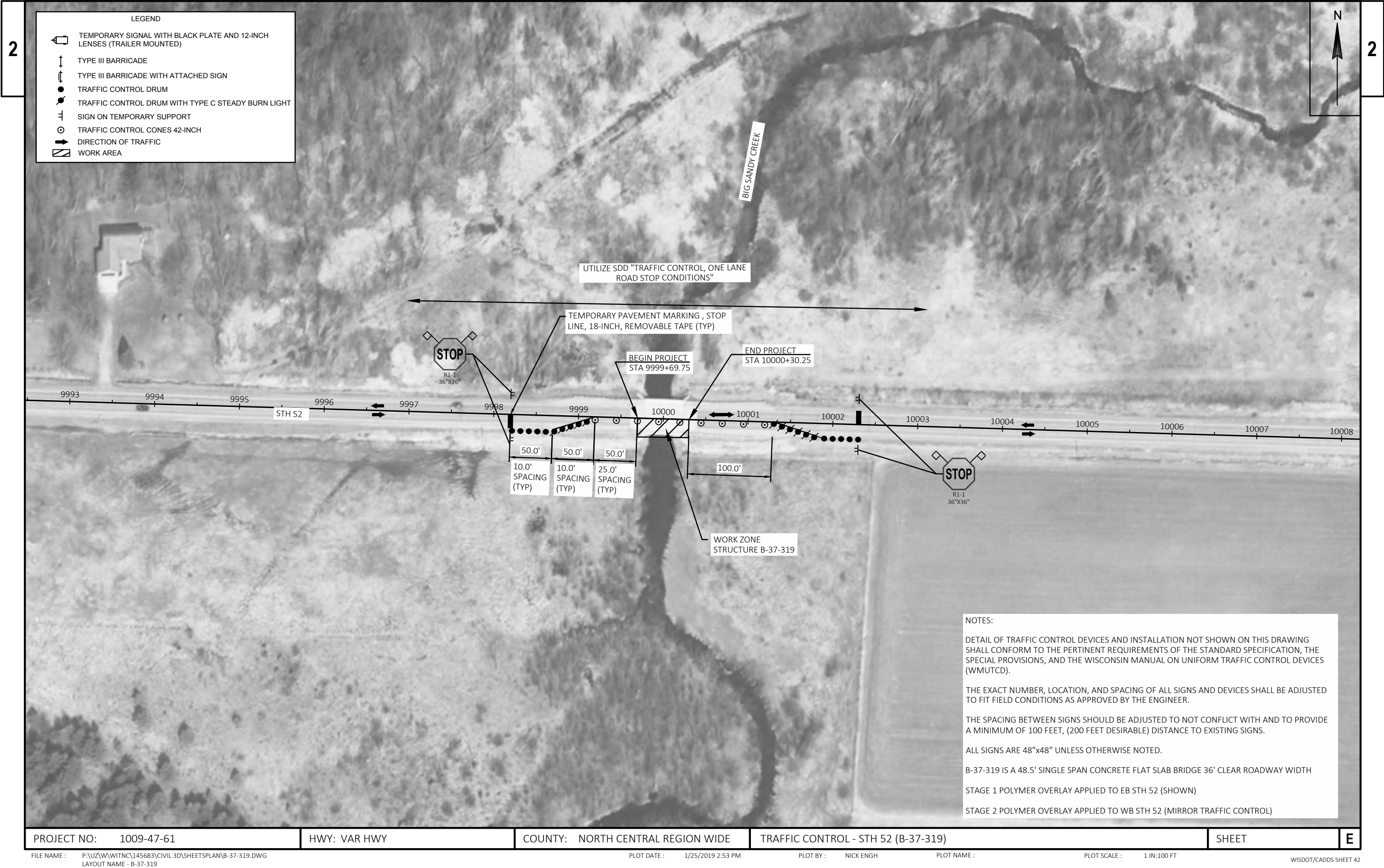
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-37-318 IS A 44.5' SINGLE SPAN CONCRETE FLAT SLAB BRIDGE 36' CLEAR ROADWAY WIDTH

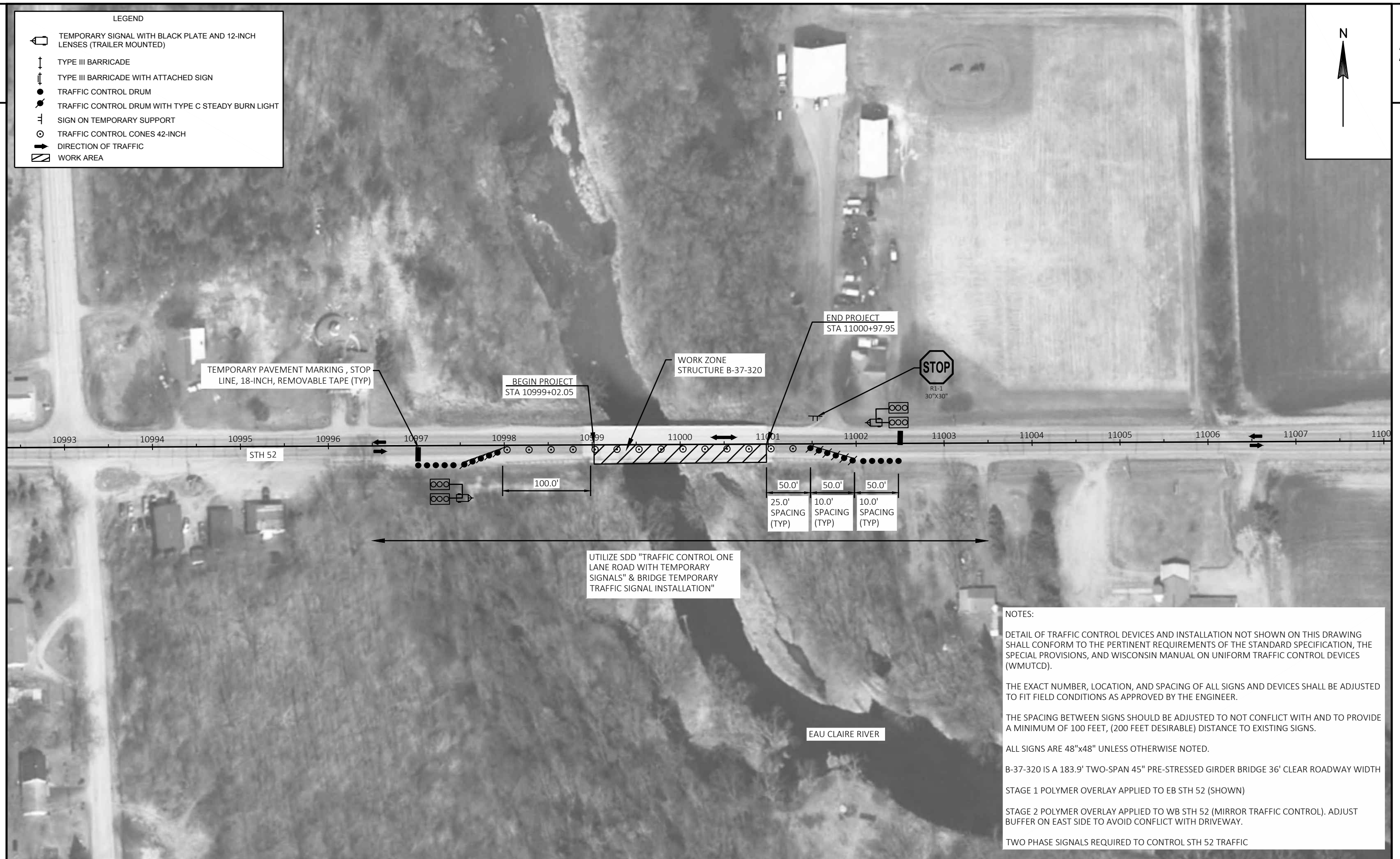
STAGE 1 POLYMER OVERLAY APPLIED TO EB STH 52 (SHOWN)

STAGE 2 POLYMER OVERLAY APPLIED TO WB STH 52 (MIRROR TRAFFIC CONTROL.



LEGEND

- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL CONES 42-INCH
- DIRECTION OF TRAFFIC
- WORK AREA



PROJECT NO: 1009-47-61

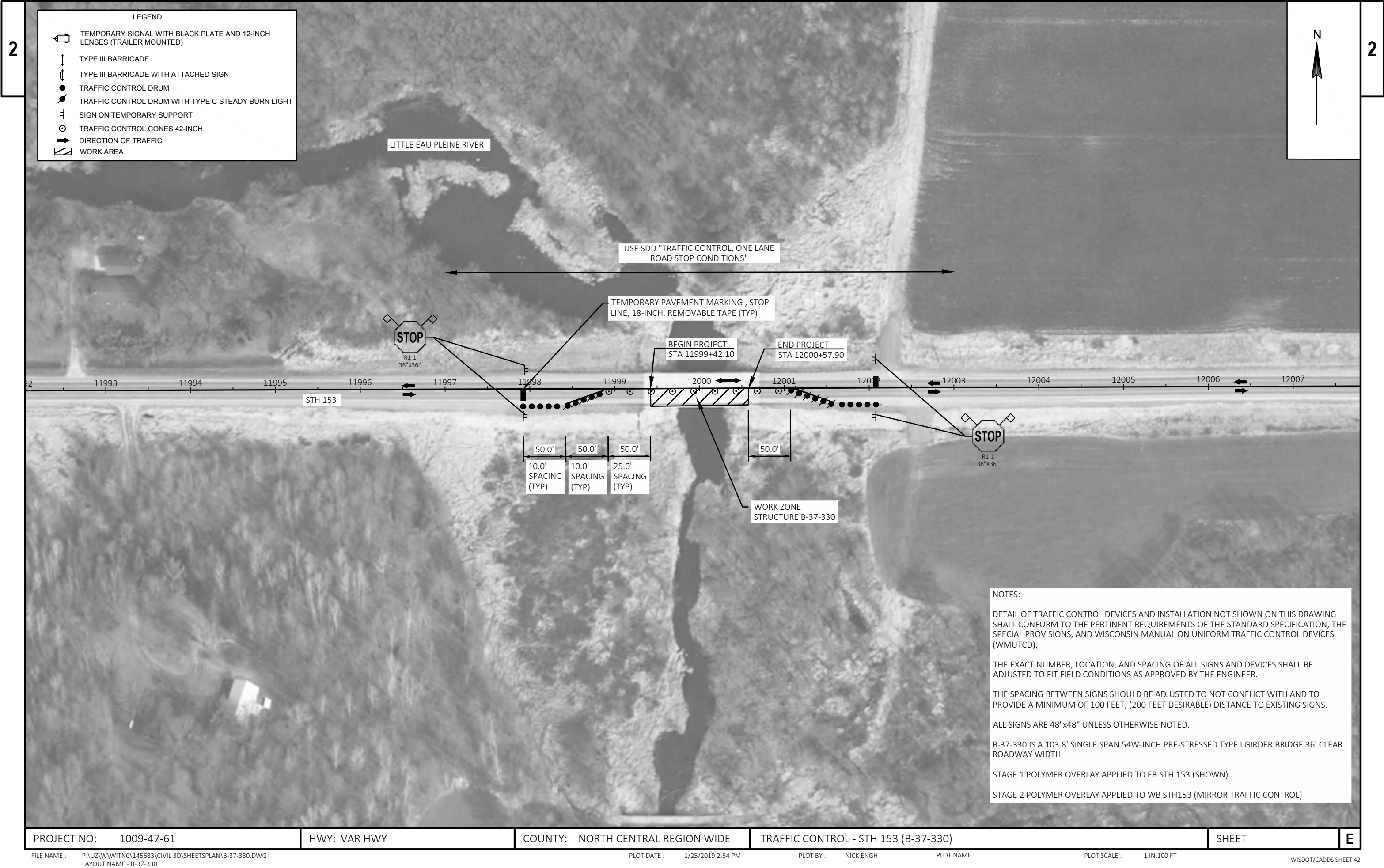
HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

TRAFFIC CONTROL - STH 52 (B-37-320)

SHEET

E



Estimate Of Quantities By Plan Sets

1009-47-61					
Line	Item	Item Description	Unit	Total	Qty
0002	509.5100.S	Polymer Overlay	SY	11,701.000	11,701.000
0004	618.0100	Maintenance And Repair of Haul Roads (project) 01. 1009-47-61	EACH	1.000	1.000
0008	619.1000	Mobilization	EACH	0.600	0.600
0010	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0012	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0016	628.7015	Inlet Protection Type C	EACH	8.000	8.000
0018	628.7570	Rock Bags	EACH	4.000	4.000
0020	642.5001	Field Office Type B 01. STH 17 & STH 52	EACH	1.000	1.000
0022	642.5001	Field Office Type B 02. STH 153	EACH	1.000	1.000
0026	643.0300	Traffic Control Drums	DAY	3,316.000	3,316.000
0028	643.0410	Traffic Control Barricades Type II	DAY	18.000	18.000
0030	643.0420	Traffic Control Barricades Type III	DAY	182.000	182.000
0032	643.0705	Traffic Control Warning Lights Type A	DAY	160.000	160.000
0034	643.0715	Traffic Control Warning Lights Type C	DAY	1,474.000	1,474.000
0036	643.0900	Traffic Control Signs	DAY	4,258.000	4,258.000
0038	643.1070	Traffic Control Cones 42-Inch	DAY	3,824.000	3,824.000
0040	643.5000	Traffic Control	EACH	0.600	0.600
0044	644.1601.S	Temporary Curb Ramp	EACH	4.000	4.000
0046	644.1810.S	Temporary Pedestrian Barricade	LF	1,510.000	1,510.000
0048	646.1020	Marking Line Epoxy 4-Inch	LF	13,745.000	13,745.000
0050	646.3020	Marking Line Epoxy 8-Inch	LF	40.000	40.000
0052	646.7120	Marking Diagonal Epoxy 12-Inch	LF	670.000	670.000
0054	646.8220	Marking Island Nose Epoxy	EACH	1.000	1.000
0056	646.9000	Marking Removal Line 4-Inch	LF	10,063.000	10,063.000
0058	646.9200	Marking Removal Line Wide	LF	270.000	270.000
0060	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	46,900.000	46,900.000
0062	649.0850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	280.000	280.000
0064	661.0100	Temporary Traffic Signals for Bridges (structure) 01. B-35-0101	LS	1.000	1.000
0066	661.0100	Temporary Traffic Signals for Bridges (structure) 02. B-37-0006	LS	1.000	1.000
0068	661.0100	Temporary Traffic Signals for Bridges (structure) 03. B-37-0306	LS	1.000	1.000
0070	661.0100	Temporary Traffic Signals for Bridges (structure) 04. B-37-0308	LS	1.000	1.000
0072	661.0100	Temporary Traffic Signals for Bridges (structure) 05. B-37-0315	LS	1.000	1.000
0074	661.0100	Temporary Traffic Signals for Bridges (structure) 06. B-37-0320	LS	1.000	1.000
0088	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000

Estimate Of Quantities By Plan Sets

					1009-47-61
0090	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000

3

FIELD OFFICE TYPE B

STATION	642.5001 EACH
STH 17 & STH 52 STRUCTURES	1
STH 153 STRUCTURES	1
ITEM TOTAL	2

MOBILIZATION

STATION	619.1000 EACH
PROJECT	0.6
ITEM TOTAL	0.6

EROSION CONTROL ITEMS

STATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628.7015 INLET PROTECTION TYPE C EACH	628.7570 ROCK BAGS EACH	REMARKS
STH 17 B-35-101	-	-	-	-	
STH 153 B-37-6	-	-	-	-	
STH 153 B-37-89	1	1	4	-	
STH 153 B-37-96	1	1	4	-	
STH 153 B-37-305	-	-	-	-	
STH 153 B-37-306	1	1	-	4	FOR FLUMES
STH 153 B-37-308	-	-	-	-	
STH 153 B-37-315	-	-	-	-	
STH 153 B-37-330	-	-	-	-	
STH 52 B-37-318	-	-	-	-	
STH 52 B-37-319	-	-	-	-	
STH 52 B-37-320	-	-	-	-	
ITEM TOTALS	3	3	8	4	

TRAFFIC CONTROL

	643.0300	643.0410	644.0420	643.0705	643.0715	643.0900	643.1070	644.1601.S	644.1810.S	646.9000	646.9200	649.0150 TEMPORARY MARKING	649.0850 TEMPORARY MARKING	661.0100 TEMPORARY TRAFFIC SIGNAL FOR BRIDGES
STATION	DRUMS DAY	BARRICADES TYPE II DAY	BARRICADES TYPE III DAY	WARNING LIGHTS TYPE A DAY	WARNING LIGHTS TYPE C DAY	SIGNS DAY	CONES 42-INCH EACH	TEMPORARY CURB RAMP EACH	TEMPORARY PEDESTRIAN BARRICADE LF	MARKING REMOVAL LINE 4-INCH LF	MARKING REMOVAL LINE WIDE LF	REMOVABLE TAPE LINE 4-INCH LF	REMOVABLE TAPE STOP LINE 18-INCH LF	(STRUCTURE) LS
STH 17 B-35-101	216	-	8	-	136	216	72	-	-	1060	-	2800	26	1
STH 153 B-37-6	180	-	6	-	72	126	0	-	-	515	-	2800	28	1
STH 153 B-37-89	853	9	26	40	468	676	2418	2	960	4525	140	12600	-	-
STH 153 B-37-96	697	9	26	40	234	728	1040	2	550	2125	130	6300	-	-
STH 153 B-37-305	126	-	6	-	66	240	48	-	-	100	-	2800	28	-
STH 153 B-37-306	344	-	8	-	96	160	0	-	-	488	-	2800	28	1
STH 153 B-37-308	216	-	72	80	72	168	0	-	-	150	-	2800	42	1
STH 153 B-37-315	186	-	6	-	72	120	0	-	-	112.5	-	2800	28	1
STH 153 B-37-330	126	-	6	-	66	240	54	-	-	100	-	2800	28	-
STH 52 B-37-318	120	-	6	-	60	132	54	-	-	100	-	2800	24	-
STH 52 B-37-319	126	-	6	-	66	1332	54	-	-	100	-	2800	24	-
STH 52 B-37-320	126	-	6	-	66	120	84	-	-	688	-	2800	24	1
ITEM TOTALS	3316	18	182	160	1474	4258	3824	4	1510	10063	270	46900	280	6

MARKING LINE

STATION	LOCATION	646.1020 LINE EPOXY 4-INCH (WHITE) LF	646.1020 LINE EPOXY 4-INCH (YELLOW) LF	646.302 LINE EPOXY 8-INCH (SKIP YELLOW) LF	646.7120 DIAGONAL EPOXY 12-INCH (DOUBLE YELLOW) LF	646.8220 ISLAND NOSE EPOXY (SKIP WHITE) LF	646.8220 ISLAND NOSE EPOXY (YELLOW) LF	646.8220 ISLAND NOSE EPOXY (YELLOW) EACH
STH 17 B-35-101	LT & RT	170	325	87.5	650	-	-	-
STH 153 B-37-6	LT & RT	140	415	100	-	-	-	-
STH 153 B-37-89	LT & RT	2550	-	-	1200	40	420	30
STH 153 B-37-96	LT & RT	1300	-	-	2800	-	180	40
STH 153 B-37-305	LT & RT	90	-	100	-	-	-	-
STH 153 B-37-306	LT & RT	820	300	187.5	-	-	-	-
STH 153 B-37-308	LT & RT	190	-	150	-	-	-	-
STH 153 B-37-315	LT & RT	180	-	112.5	-	-	-	-
STH 153 B-37-330	LT & RT	240	-	100	-	-	-	-
STH 52 B-37-318	LT & RT	120	-	100	-	-	-	-
STH 52 B-37-319	LT & RT	130	-	100	-	-	-	-
STH 52 B-37-320	LT & RT	400	550	137.5	-	-	-	-
SUBTOTAL		6330	1590	1175	4650	40	600	70
ITEM TOTALS				13745		40	670	1

NOTE: ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR
ENGINEER ESTIMATE CATEGORY 0010 UNLESS OTHERWISE NOTED

PROJECT NO: 1009-47-61

HWY: VAR HWY

COUNTY: NORTH CENTRAL REGION WIDE

MISCELLANEOUS QUANTITIES

SHEET

E

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
09G02-05C	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
15C02-07F	ADVANCED WIDTH RESTRICTION SIGNING
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C06-09	SIGNING & MARKING FOR TWO LANE BRIDGES
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15C11-07A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15D28-03	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-04A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-04B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-04C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D32-05	TRAFFIC CONTROL, ONE LANE ROAD STOP CONDITION
15D33-05	TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



INLET PROTECTION, TYPE A

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

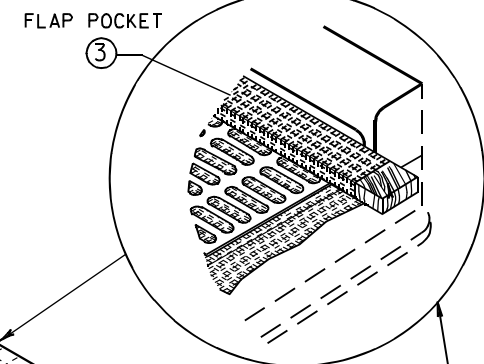
TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)



USE REBAR OR STEEL ROD FOR REMOVAL OR
FOR INLETS WITH CAST CURB BOX USE WOOD 2" X 4", EXTEND 10" BEYOND GRATE WIDTH ON BOTH SIDES, LENGTH VARIES. SECURE TO GRATE WITH WIRE OR PLASTIC TIES

MINIMUM DOUBLE STITCHED SEAMS ALL AROUND SIDE PIECES AND ON FLAP POCKETS.

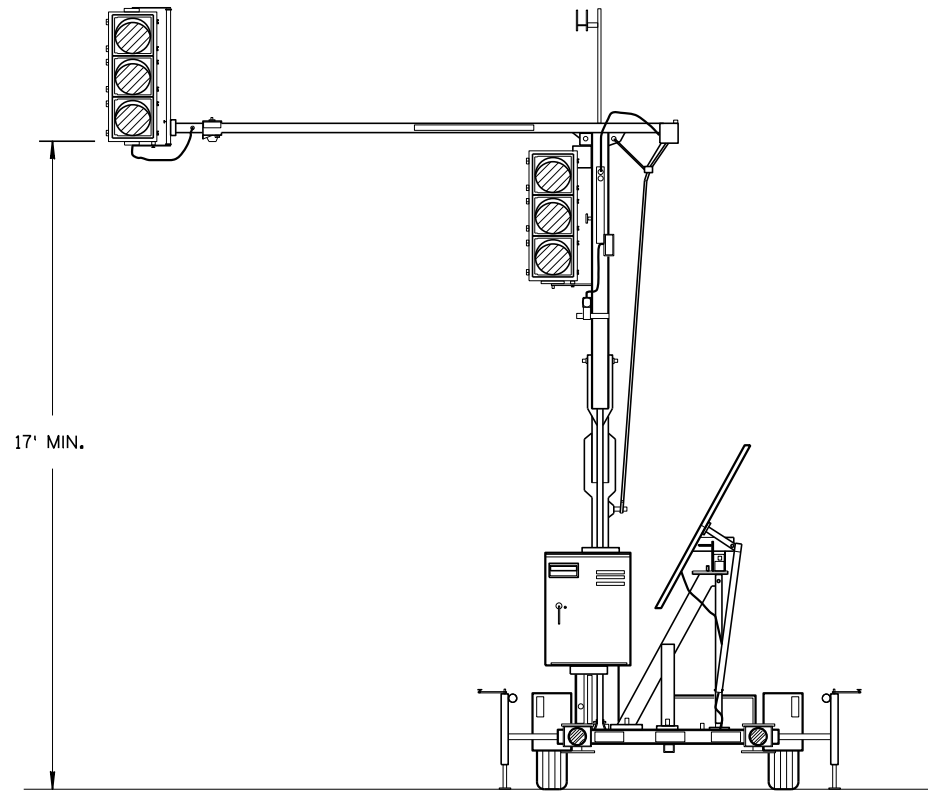
WOOD 2" X 4" EXTENDS 8" BEYOND GRATE WIDTH ON BOTH SIDES, LENGTH VARIES. SECURE TO GRATE WITH WIRE OR PLASTIC TIES

4" X 6" OVAL HOLE SHALL BE HEAT CUT INTO ALL FOUR SIDE PANELS.

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Cannestra
DATE
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER

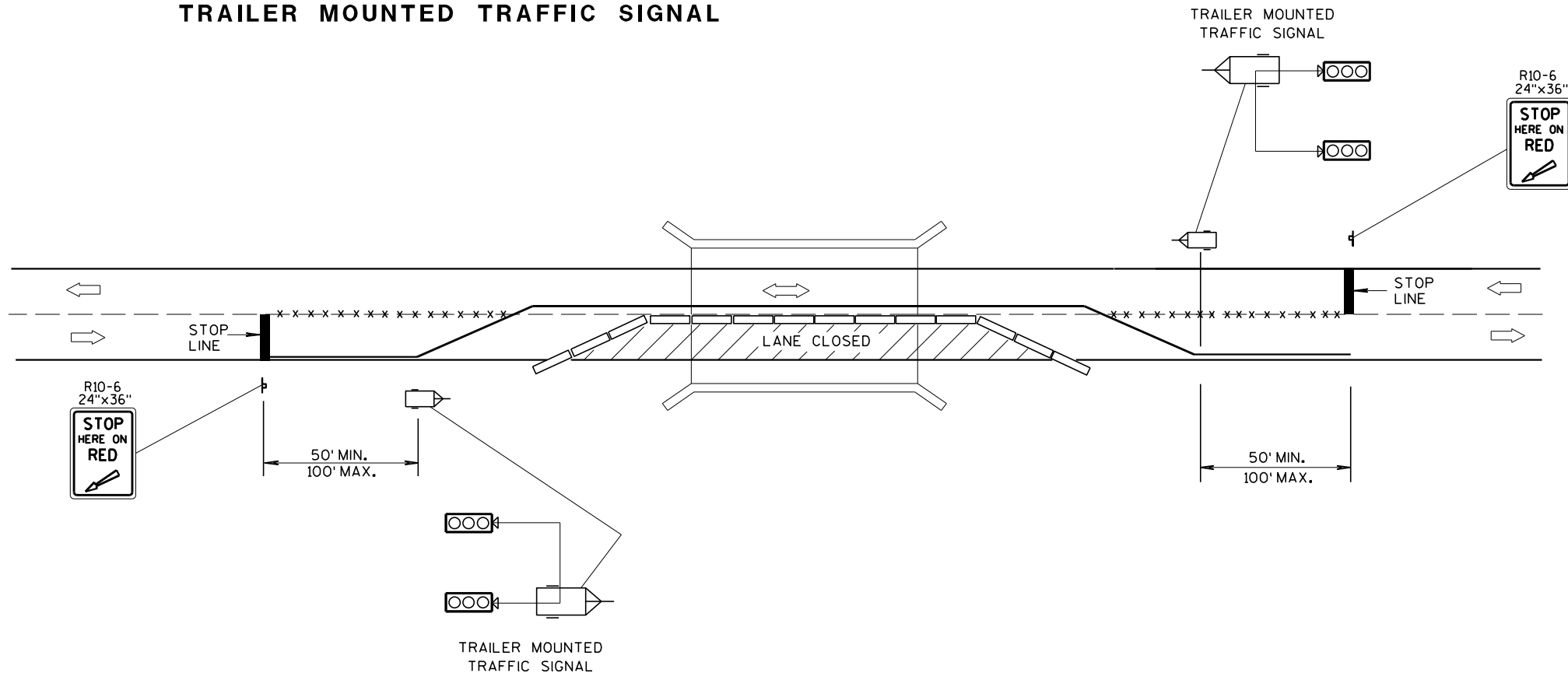


TRAILER MOUNTED TRAFFIC SIGNAL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15 D 33.



TYPICAL TRAILER MOUNTED TRAFFIC SIGNAL LOCATION

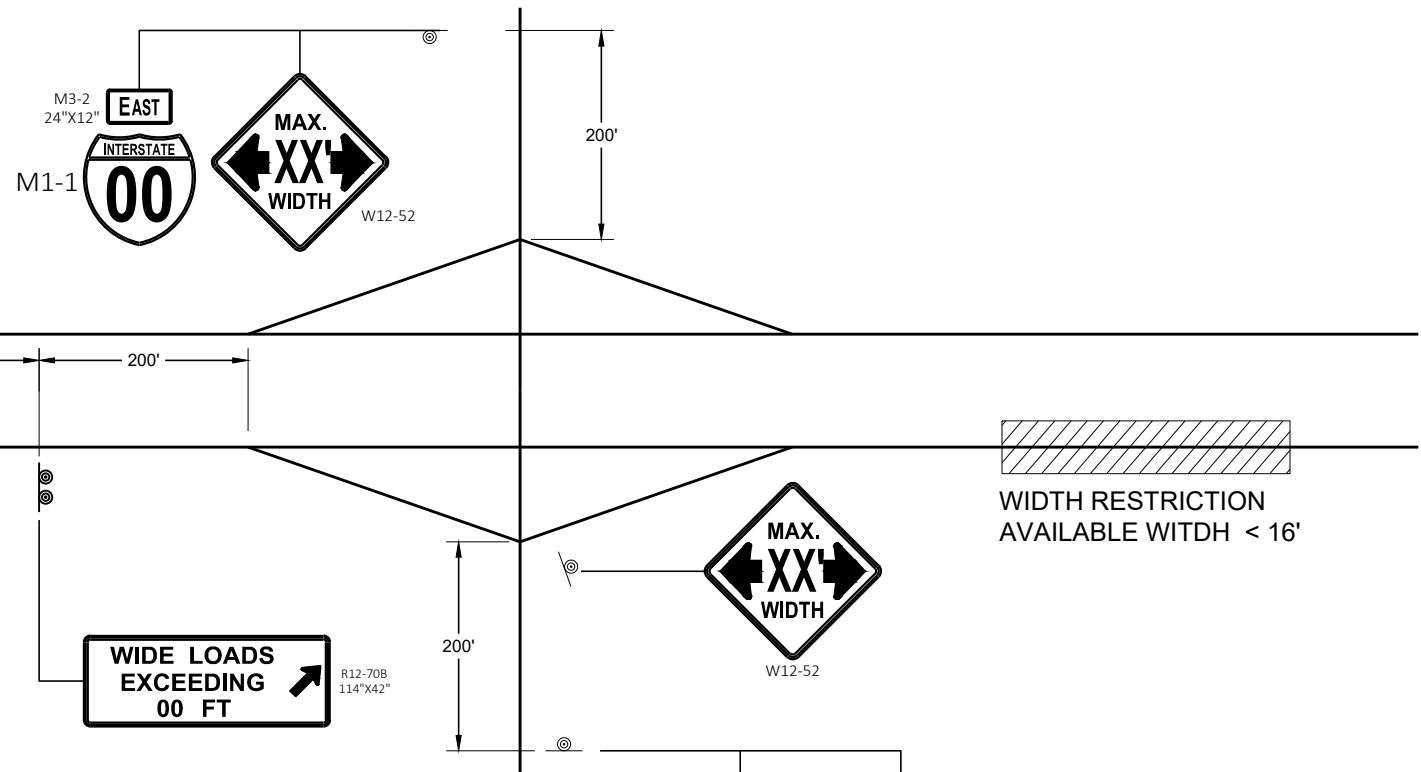
LEGEND

- POST MOUNTED SIGN
- REMOVING PAVEMENT MARKING
- TEMPORARY PRECAST CONCRETE BARRIER
- TRAILER MOUNTED TRAFFIC SIGNAL
- DIRECTION OF TRAFFIC FLOW

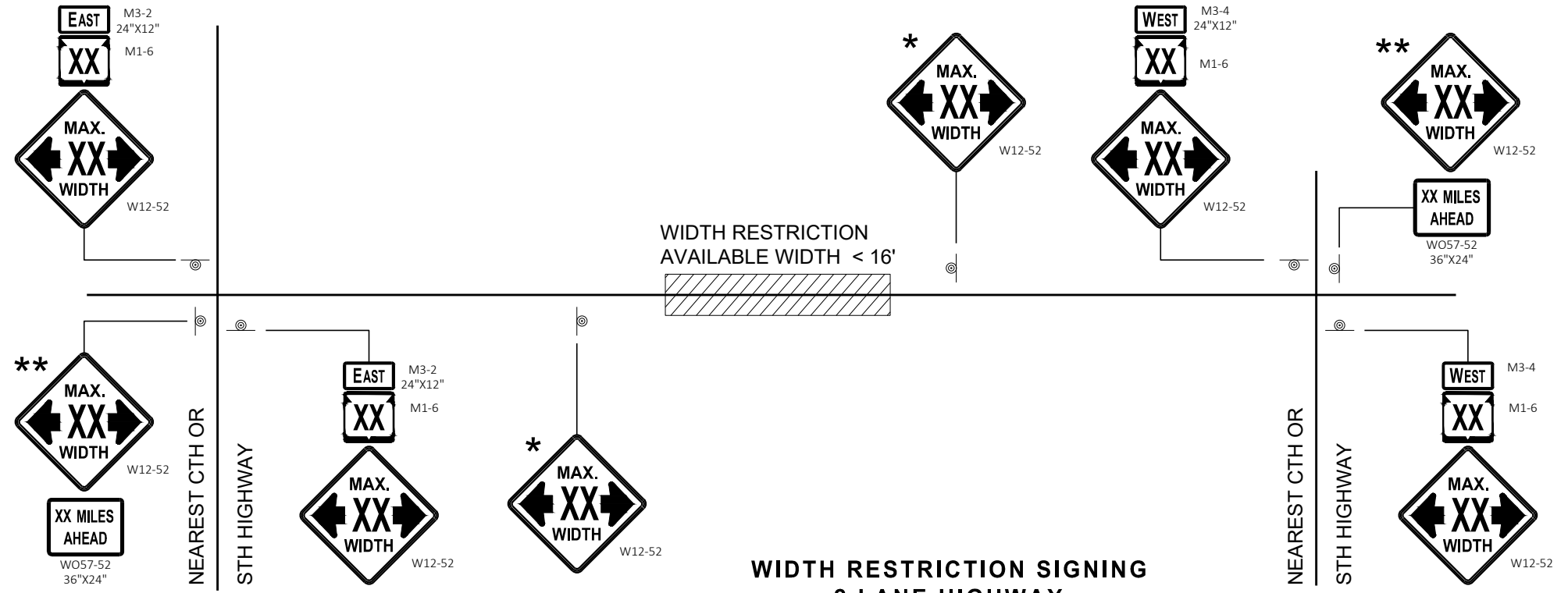
BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Ahmet Demirbilek
DATE STATE ELECTRICAL ENGINEER
FHWA



WIDTH RESTRICTION SIGNING



WIDTH RESTRICTION SIGNING
2 LANE HIGHWAY

LEGEND

 SIGN ON PERMANENT SUPPORT

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WIDTH ON SIGN TO BE APPROXIMATELY ONE FOOT LESS THAN AVAILABLE WIDTH.

* PLACE 500 FEET BEFORE THE W20 - 1A AND 500 FEET BEFORE ADDITIONAL SIGNS FOR ROADWAYS WITH A PRE - CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE. FOR 35-40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25-30 MPH, USE 200 FOOT TYPICAL SPACING.

** SIGN SHALL BE VISIBLE FROM ROADWAY.

*** ADDITIONAL SIGNS NEEDED IF THERE IS AN ON RAMP BETWEEN SIGNS.

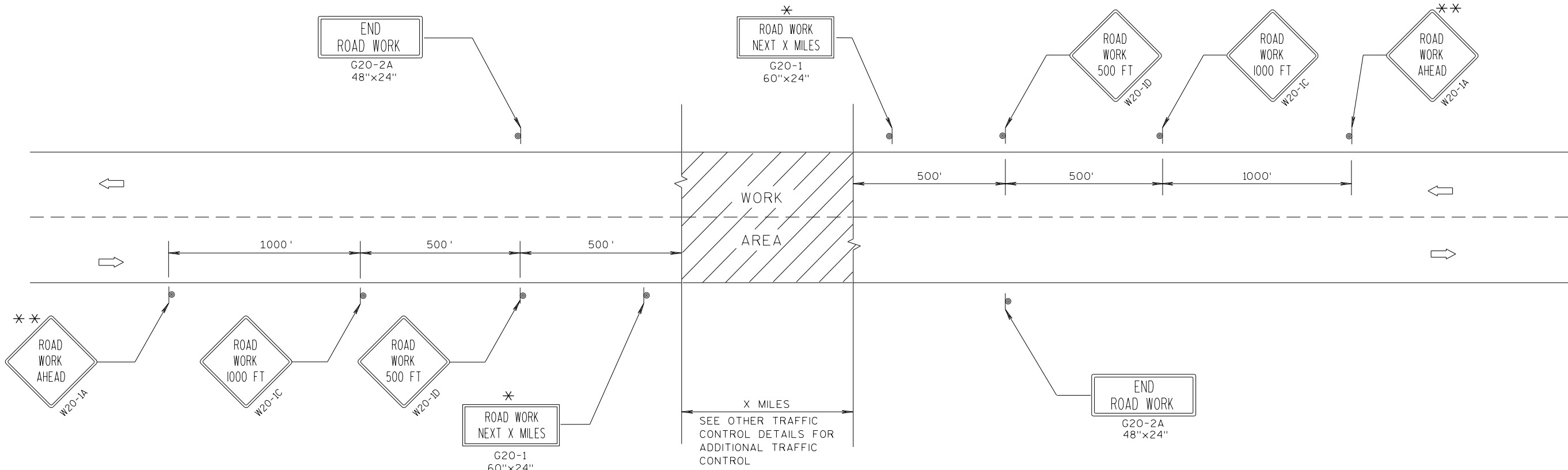


WIDTH ON SIGN TO BE APPROX. 1 - FOOT LESS THAN AVAILABLE WIDTH

ADVANCED WIDTH
RESTRICTION SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

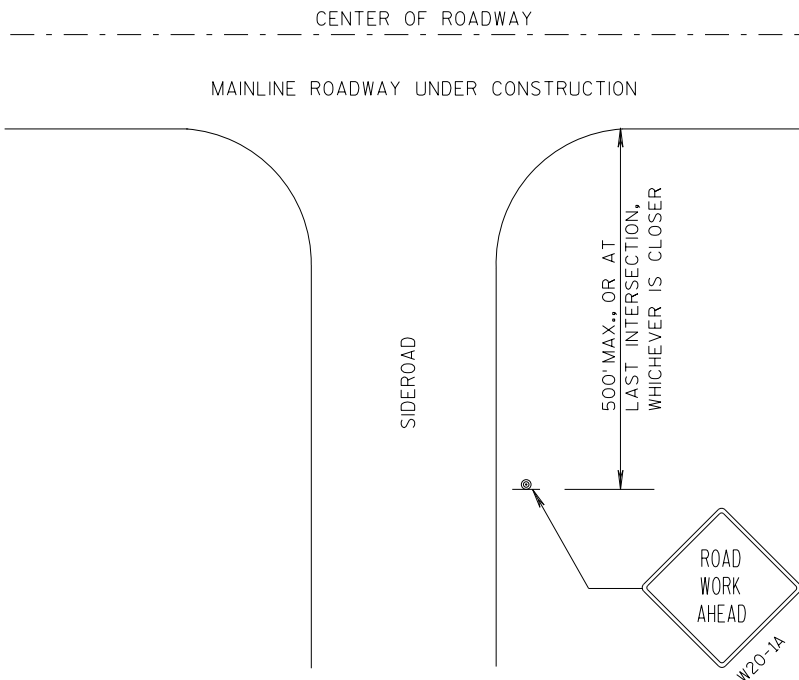
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

* * PLACE ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



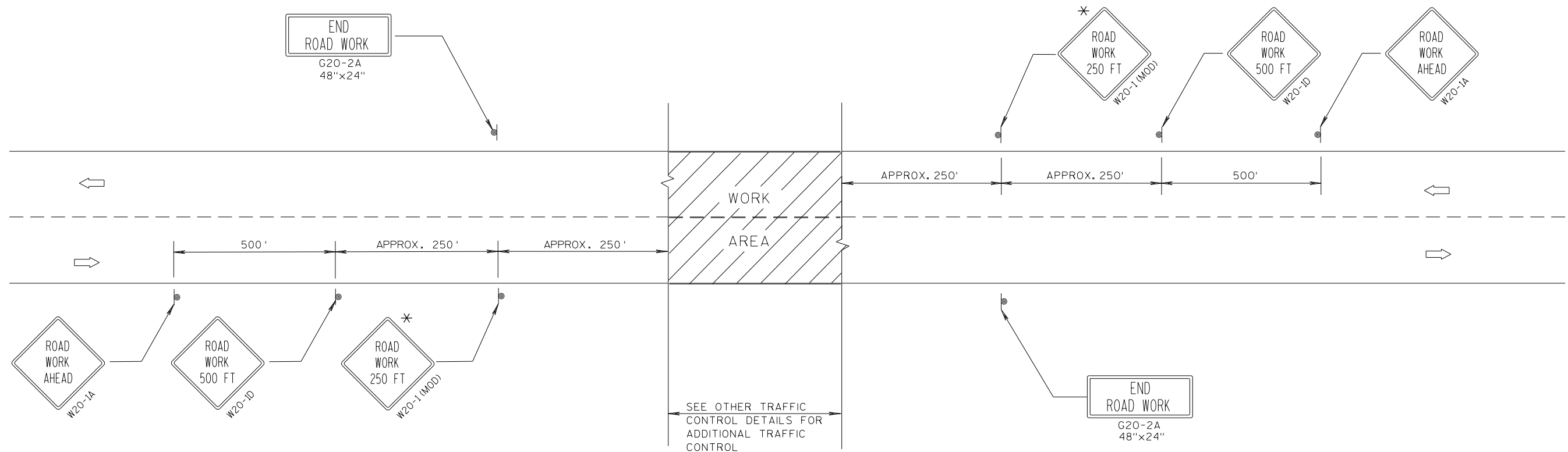
LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 45 M.P.H.
OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

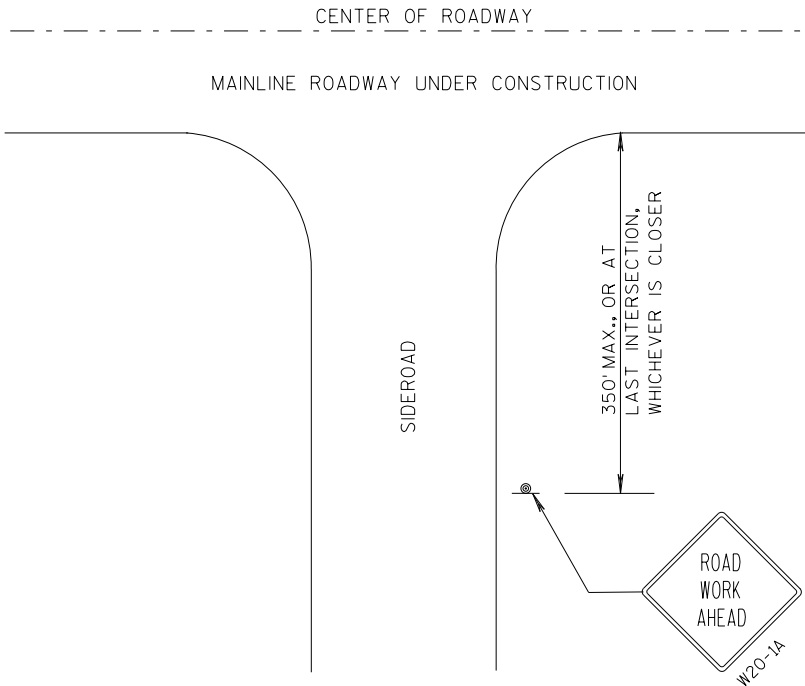
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

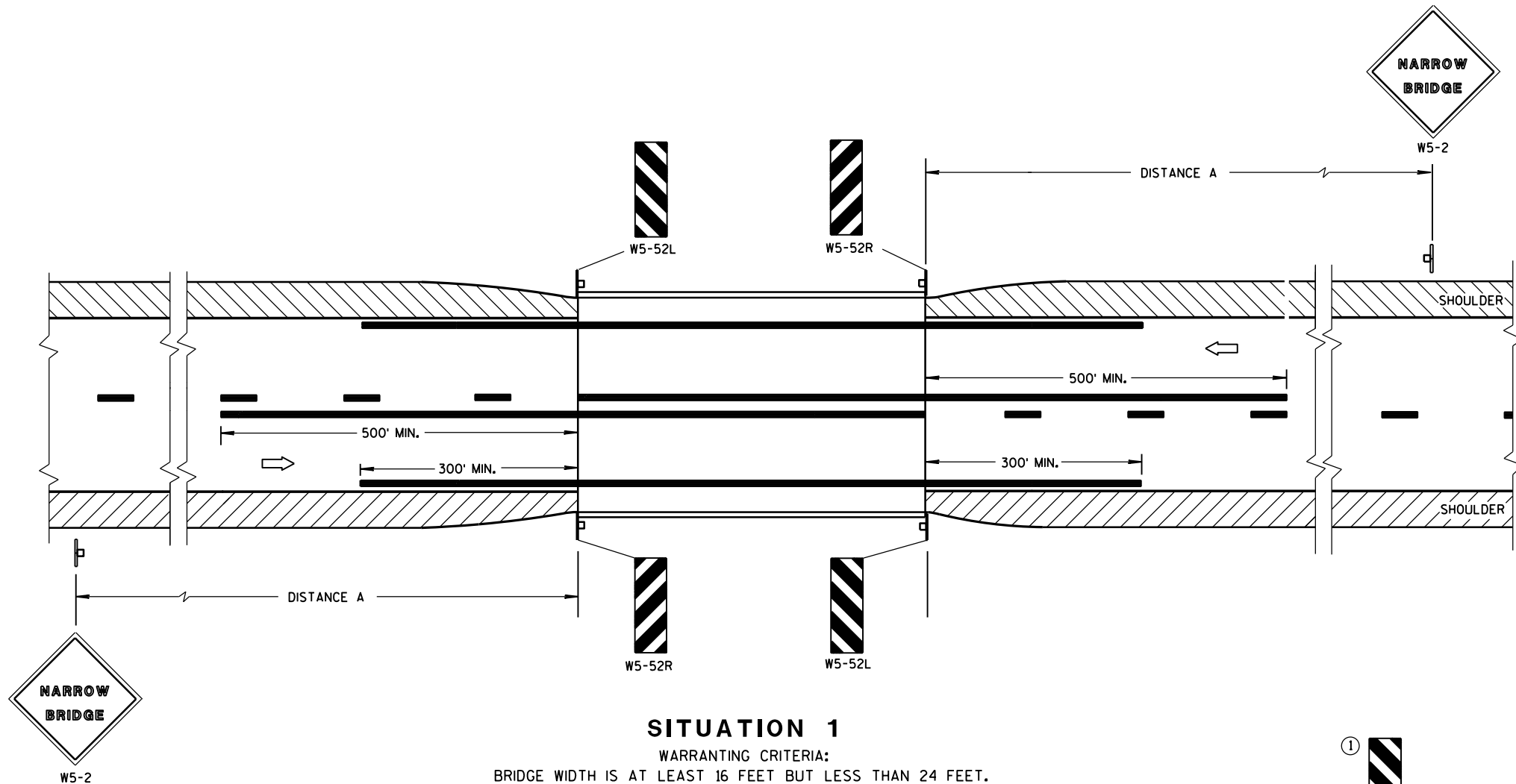
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

TRAFFIC CONTROL. ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A "
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

GENERAL NOTES

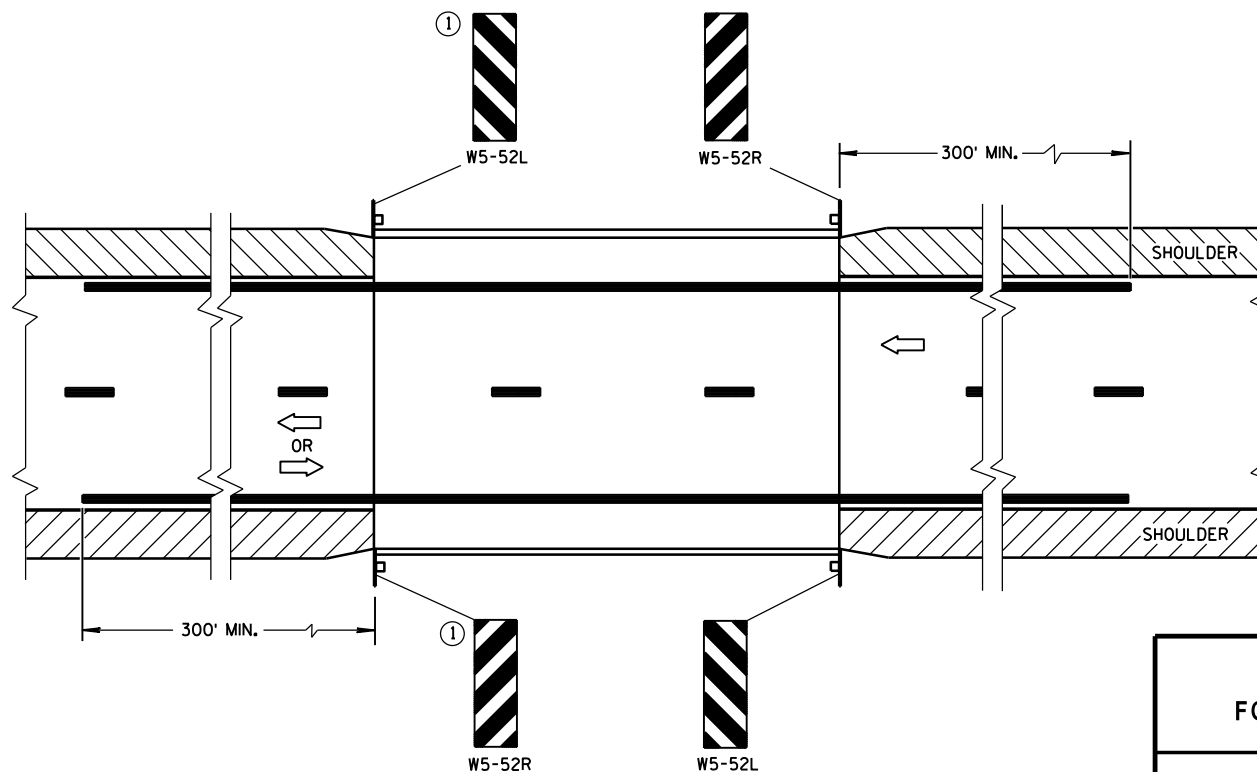
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

① OMIT ON ONE-WAY TRAVELLED WAYS.

➡ DIRECTION OF TRAFFIC



SITUATION 2

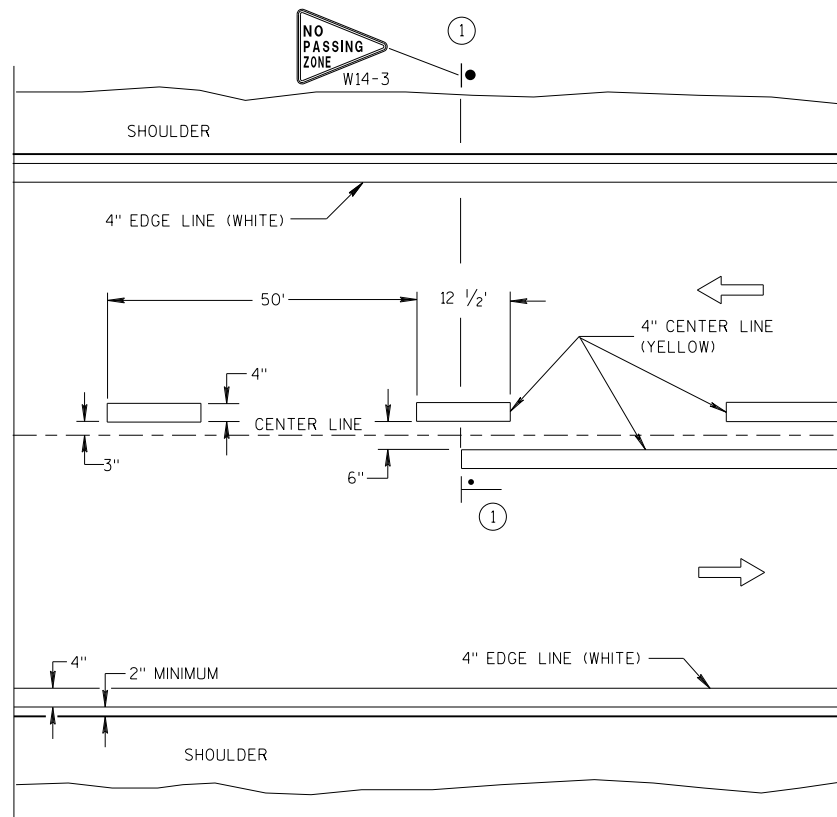
WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

SIGNING & MARKING FOR TWO LANE BRIDGES

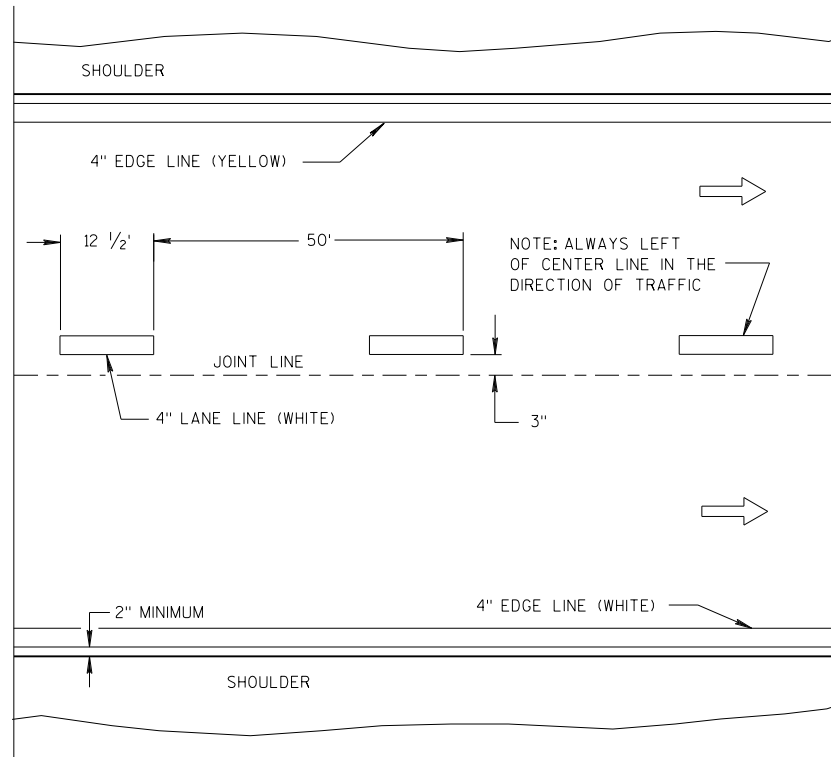
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

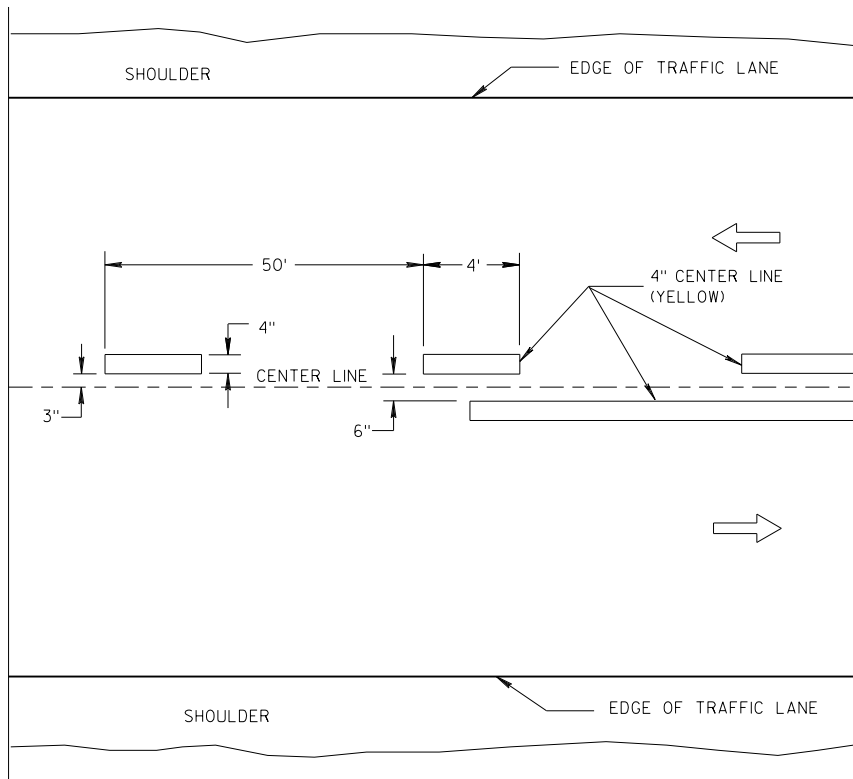


TWO WAY TRAFFIC

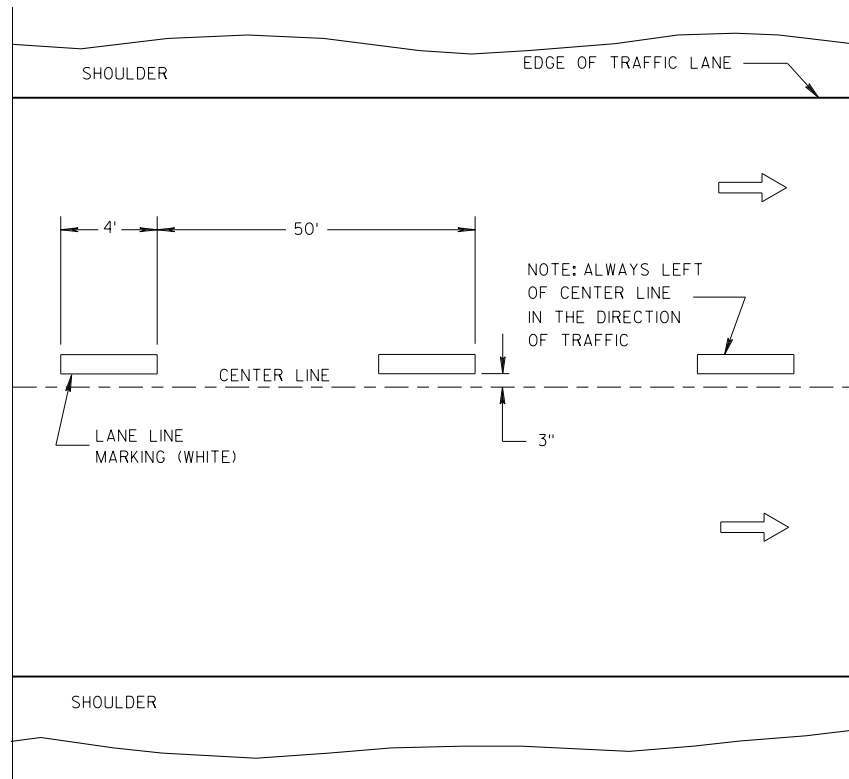


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

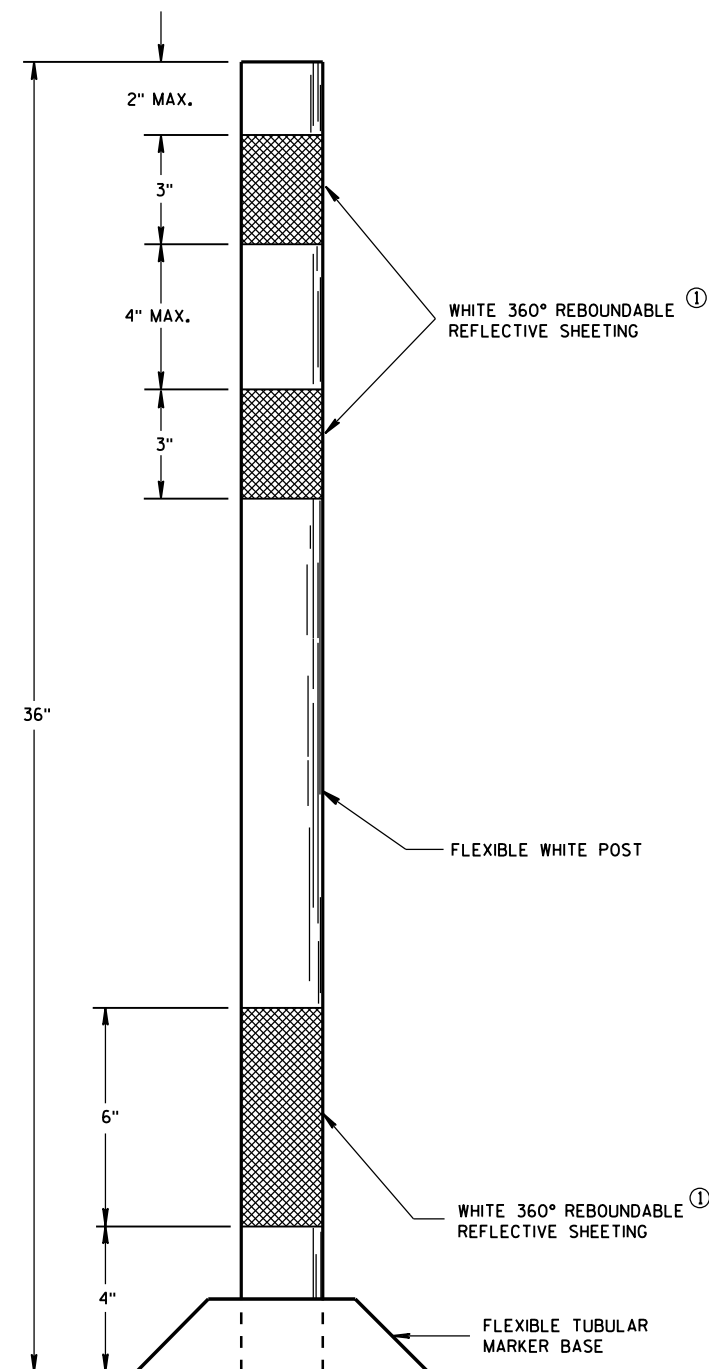
LEGEND

- "T" MARKING
- POST MOUNTED SIGN

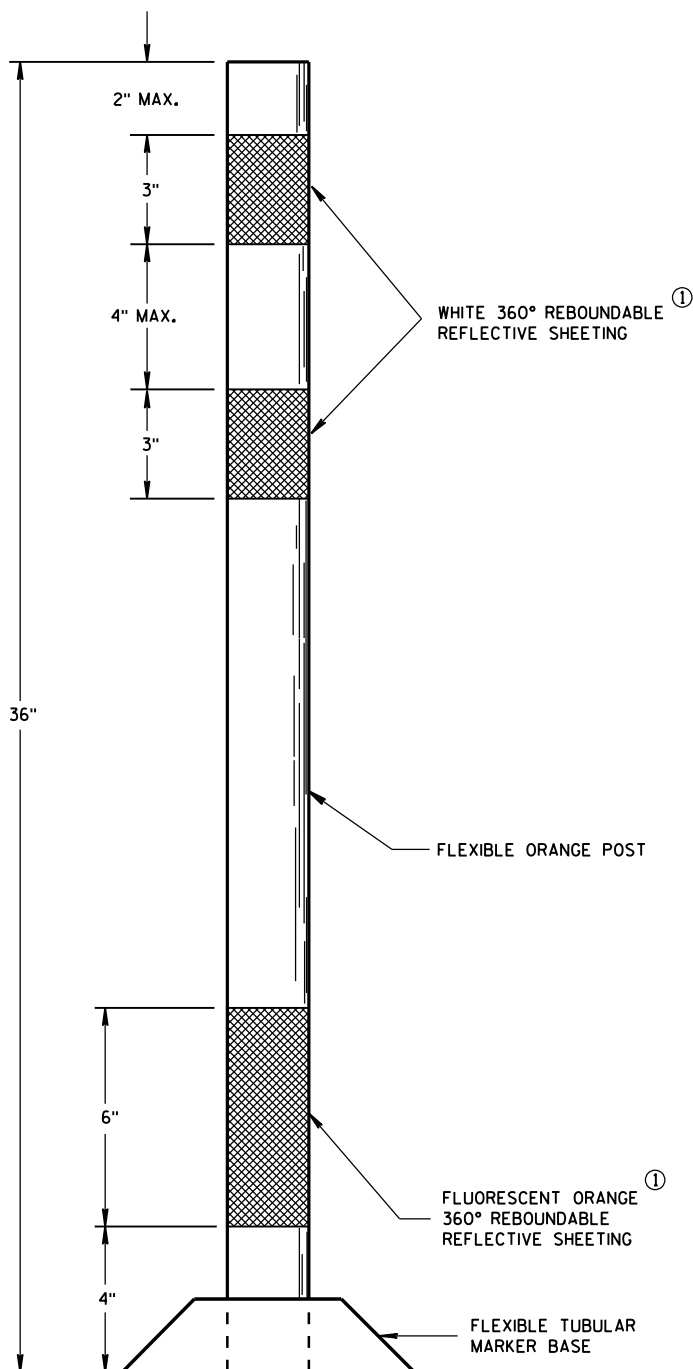
LONGITUDINAL MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA



**FLEXIBLE
TUBULAR MARKER POST
PERMANENT CROSSOVER**



**FLEXIBLE
TUBULAR MARKER POST
WORK ZONE**

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST FOUNDATION WHEN SECURED TO THE PAVEMENT.

THE ASPHALTIC ADHESIVE OR BUTYL PAD FURNISHED SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS, UNLESS DIRECTED BY THE ENGINEER TO USE BOLTS.

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

**CHANNELIZING DEVICES
FLEXIBLE TUBULAR MARKER
POST**

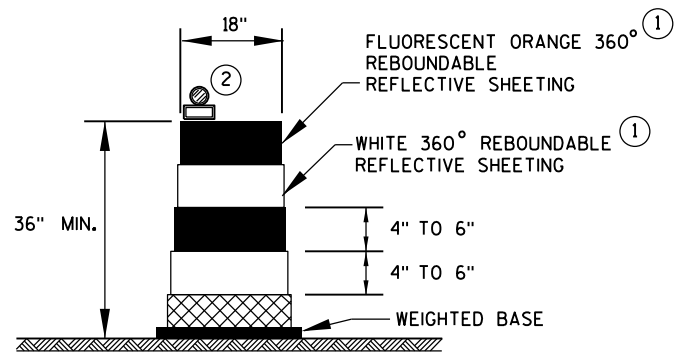
**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

APPROVED

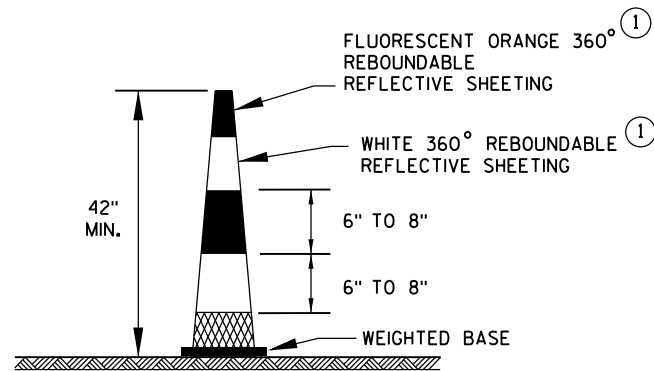
June 2017
DATE

/S/ Andrew Heldtke
WORK ZONE ENGINEER

FHWA



DRUM

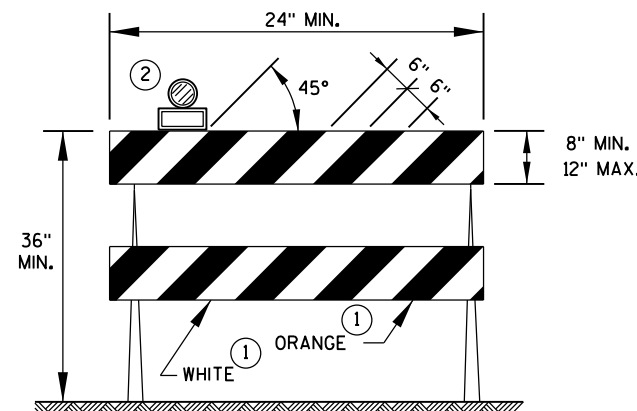


42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS

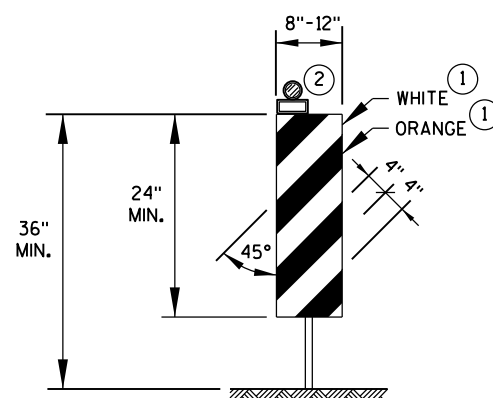
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



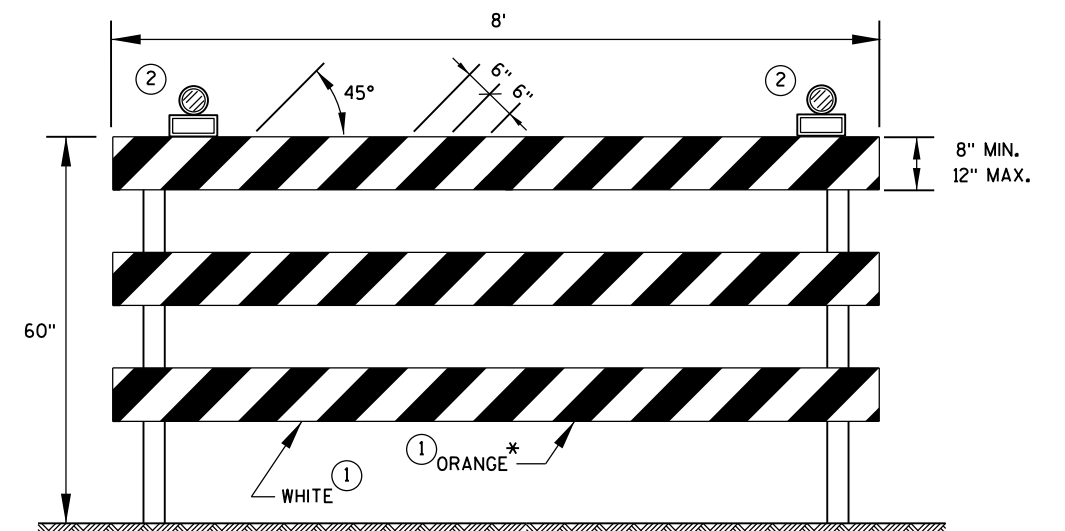
TYPE 2 BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE 3 BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017
DATE

FHWA

/S/ Andrew Heidtke
WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

TABLE A

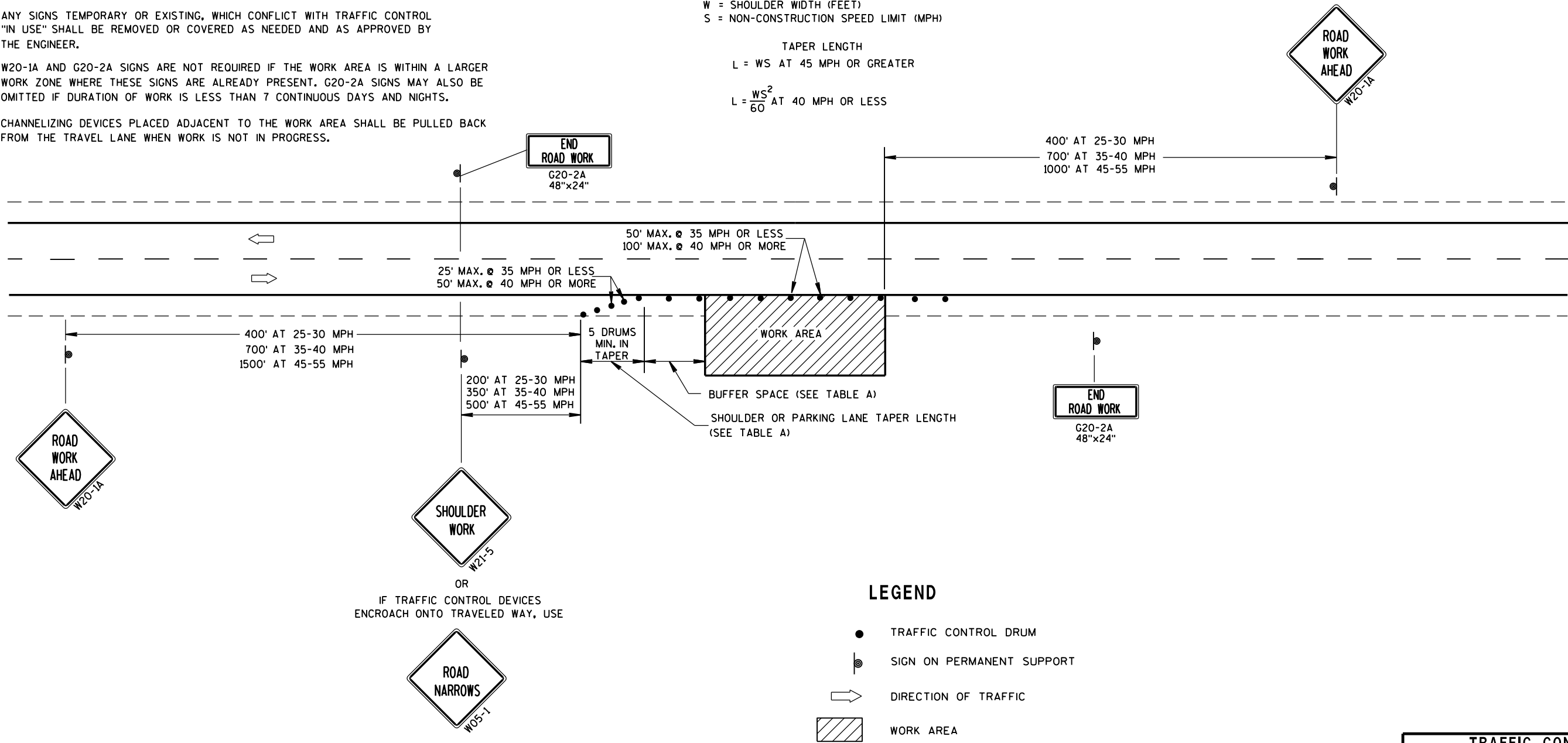
SHOULDER TAPER LENGTH (FEET)					BUFFER SPACE (FEET)
S	W	4	6	8	
30	20	30	40	50	200
35	30	45	55	70	250
40	40	55	75	90	305
45	60	90	120	150	360
50	70	100	135	170	425
55	75	110	150	185	495

W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH
L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = $\frac{1}{3}L$



LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 14, 2015 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

6

4' MINIMUM
5' DESIRABLE

**SIDEWALK
CLOSED**

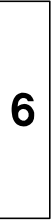
R9-9
24"x12"

①

TEMPORARY
PEDESTRIAN
CROSSING. SEE
D 15 D 30
SHEET "C".

The diagram shows a cross-section of a street with a temporary pedestrian crossing. A sidewalk on the left is closed, indicated by a hatched rectangular area labeled 'SIDEWALK CLOSED'. A callout box points to this area with the text 'R9-9 24"x12"'. A dimension line indicates a width of '4' MINIMUM' and '5' DESIRABLE'. A circular callout with the number '1' points to the closed sidewalk area. The street has two lanes with dashed center lines. Pedestrian crossing arrows (one pointing down, one pointing up) are shown in the center of the street. The crossing is marked with a series of short vertical lines across the street width. The sidewalk on the right is open and has a series of short vertical lines along its edge.

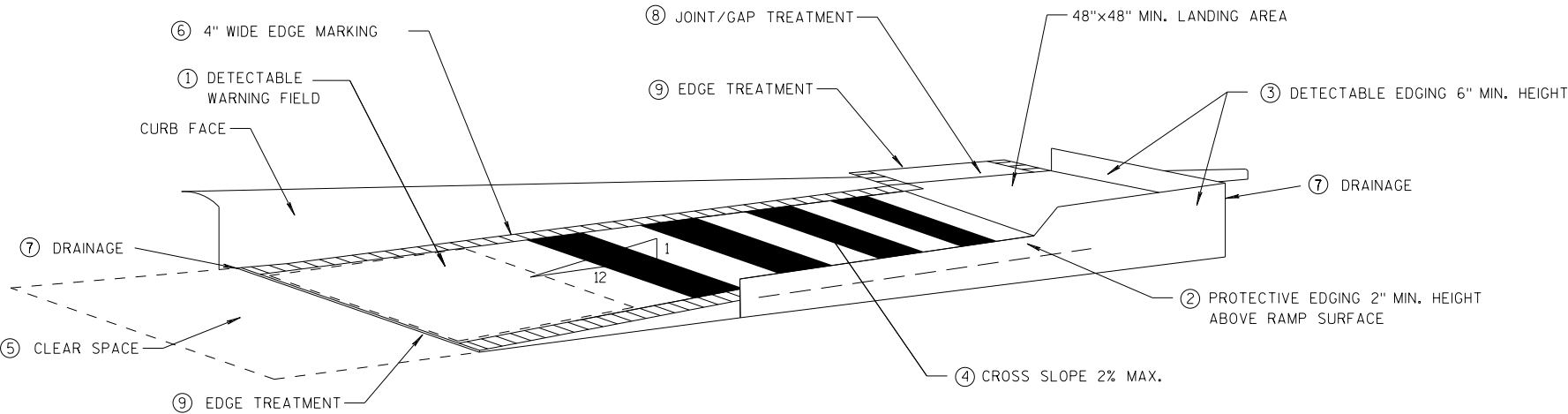
S.D.D. 15 D 30-4a



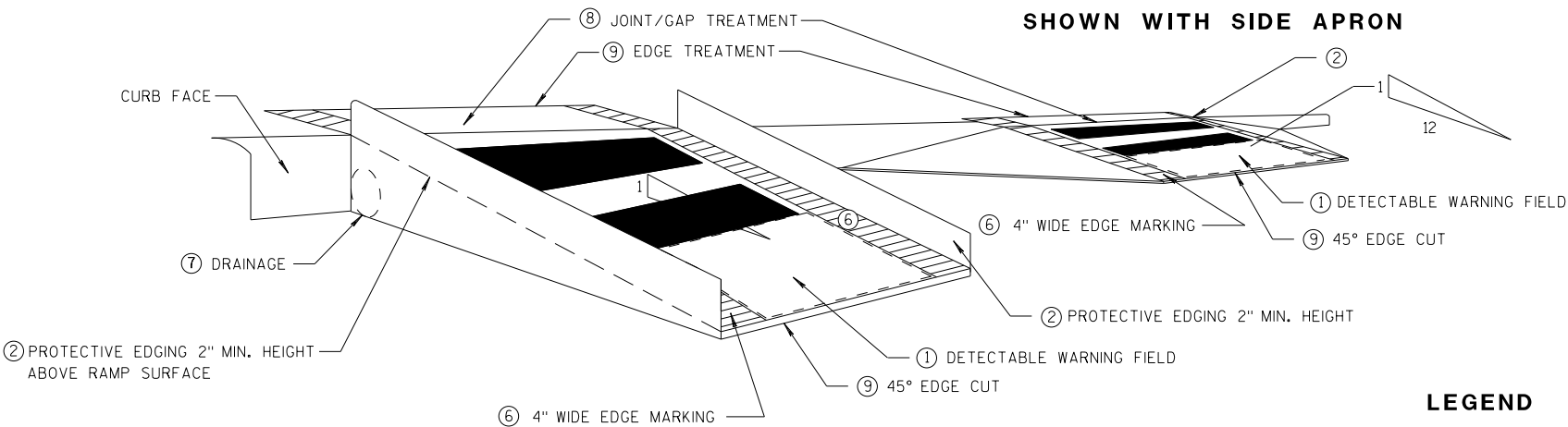
S.D.D. 15 D 30-4a

GENERAL NOTES

- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.
- 1 CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 805 SHEET "E".
 - 2 PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
 - 3 DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
 - 4 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 - 5 CLEAR SPACE OF 48"x48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 - 6 THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
 - 7 DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
 - 8 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 - 9 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
 - 10 5' WIDE MIN. WITH PEDSETRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.

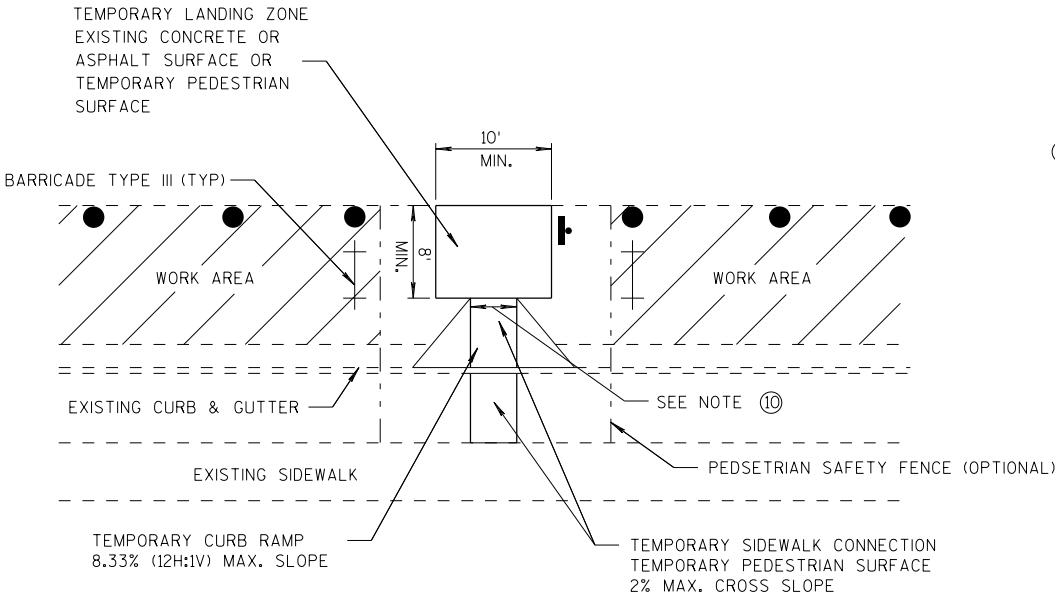


TEMPORARY CURB RAMP
PARALLEL TO CURB



SHOWN WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP
PERPENDICULAR TO CURB



TEMPORARY BUS STOP PAD

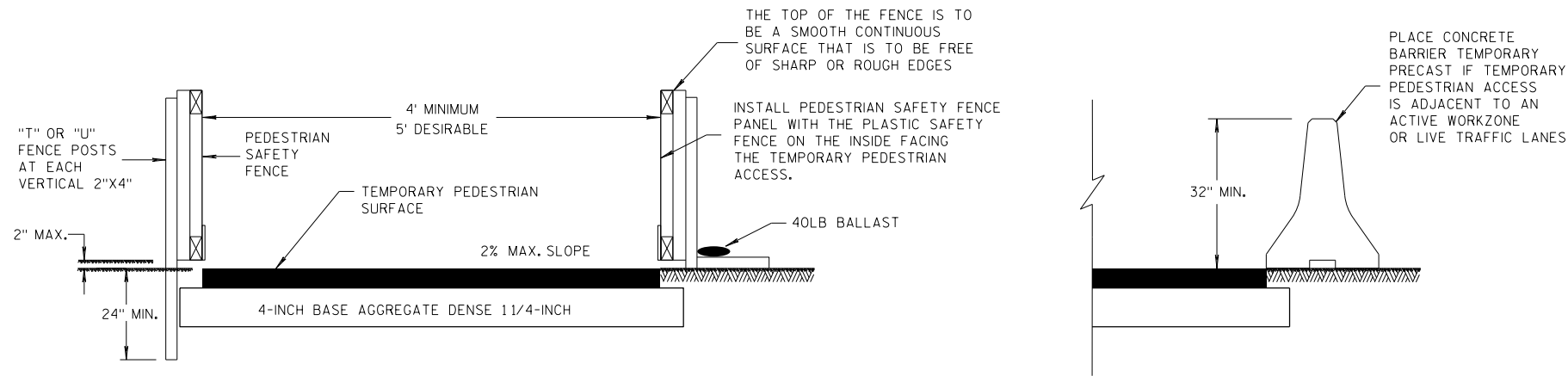
LEGEND

- WORK AREA (hatched box)
- TYPE III BARRICADE (T-shaped symbol)
- TRAFFIC CONTROL DRUM (black circle)

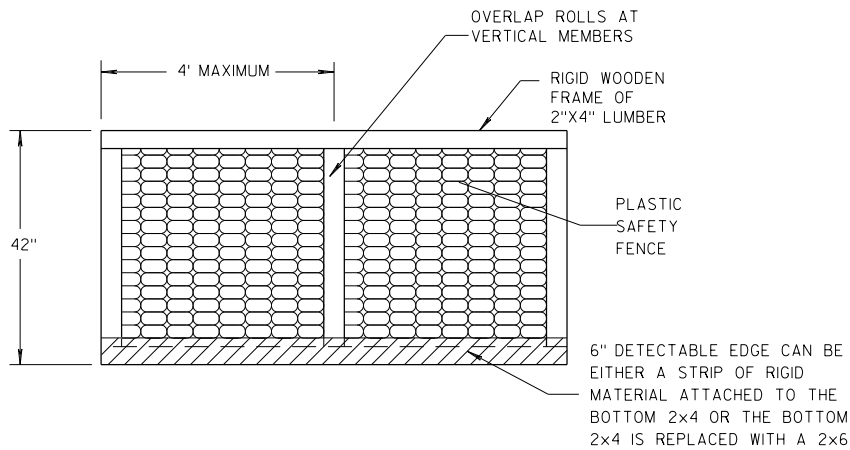
TRAFFIC CONTROL,
TEMPORARY ADA COMPLIANT
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

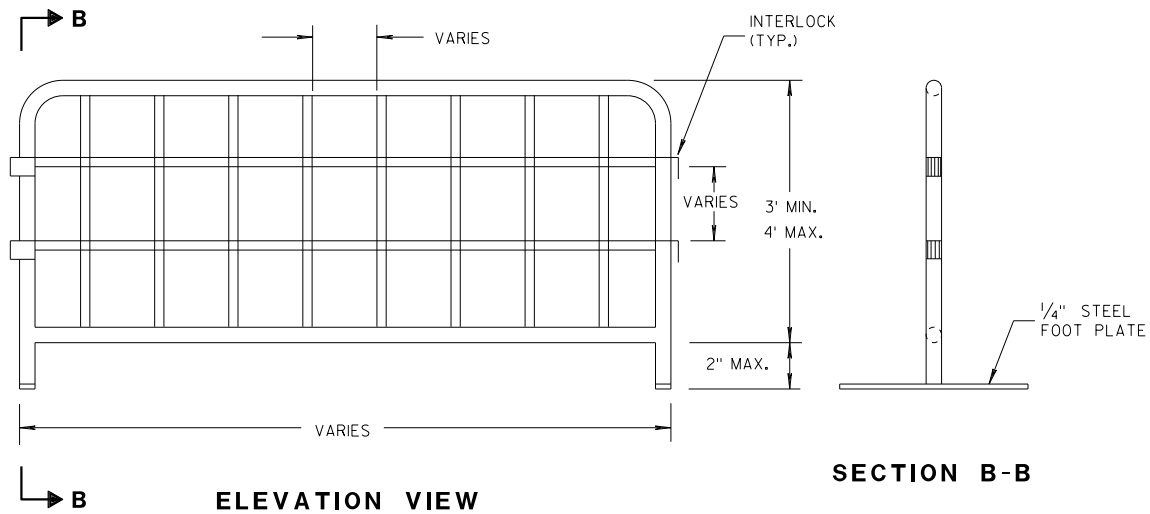
APPROVED
DATE 7/2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER
FHWA



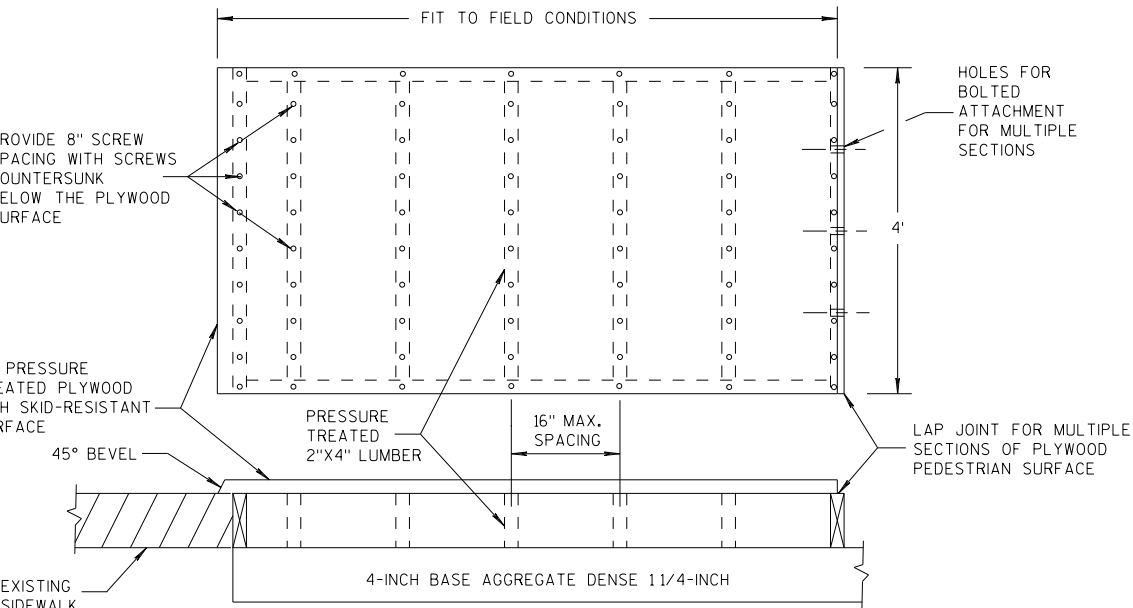
TEMPORARY PEDESTRIAN ACCESS



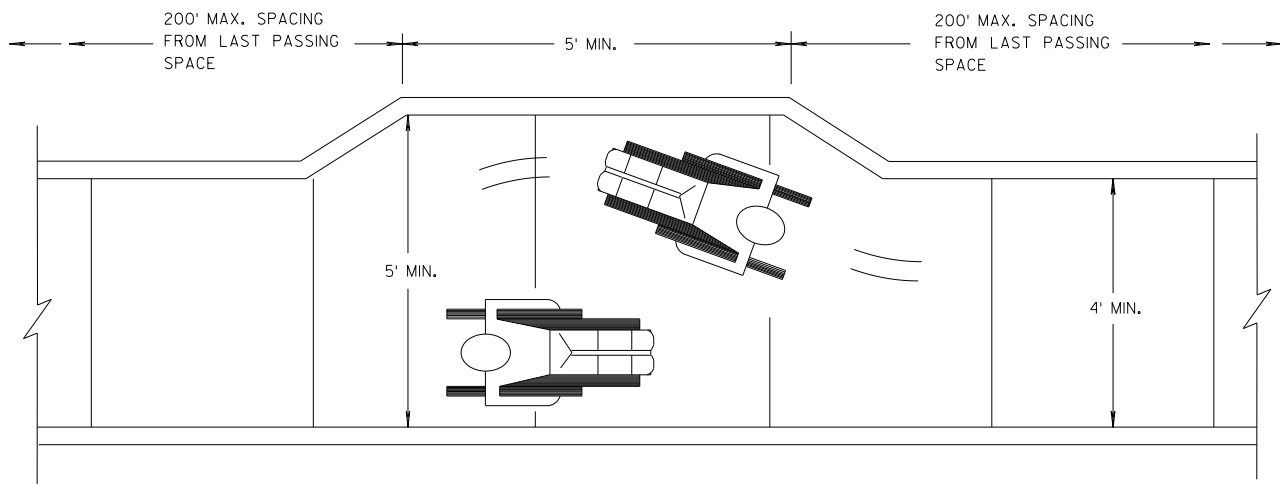
PEDESTRIAN SAFETY FENCE



TEMPORARY PEDESTRIAN STEEL BARRICADE



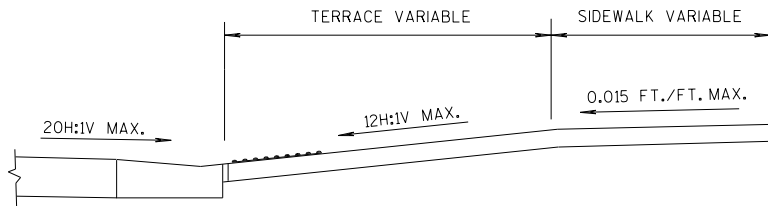
TEMPORARY PEDESTRIAN SURFACE PLYWOOD



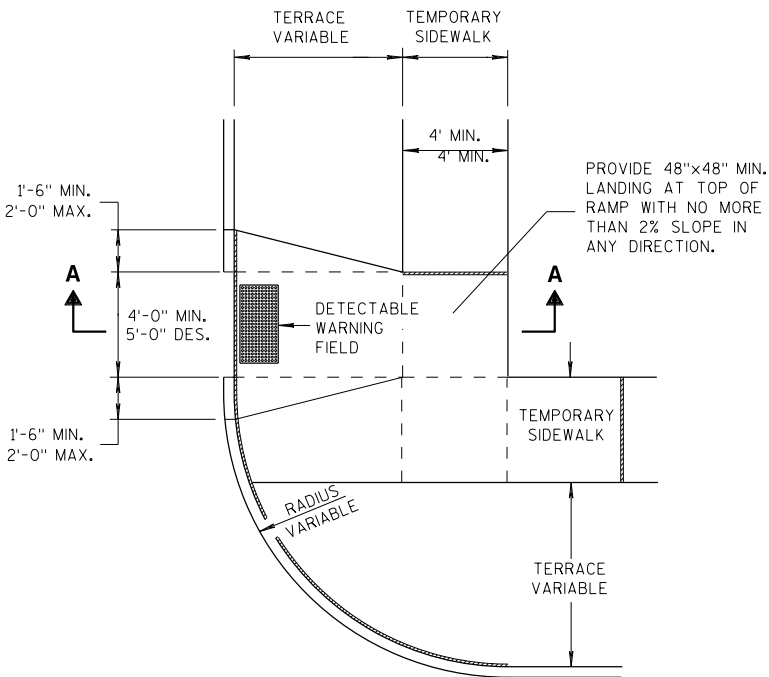
NARROW SIDEWALK PASSING DETAIL

GENERAL NOTES

- 1 INTERCHANGEABLE WITH THE PEDESTRIAN SAFETY FENCE.



SECTION A-A



TEMPORARY TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)

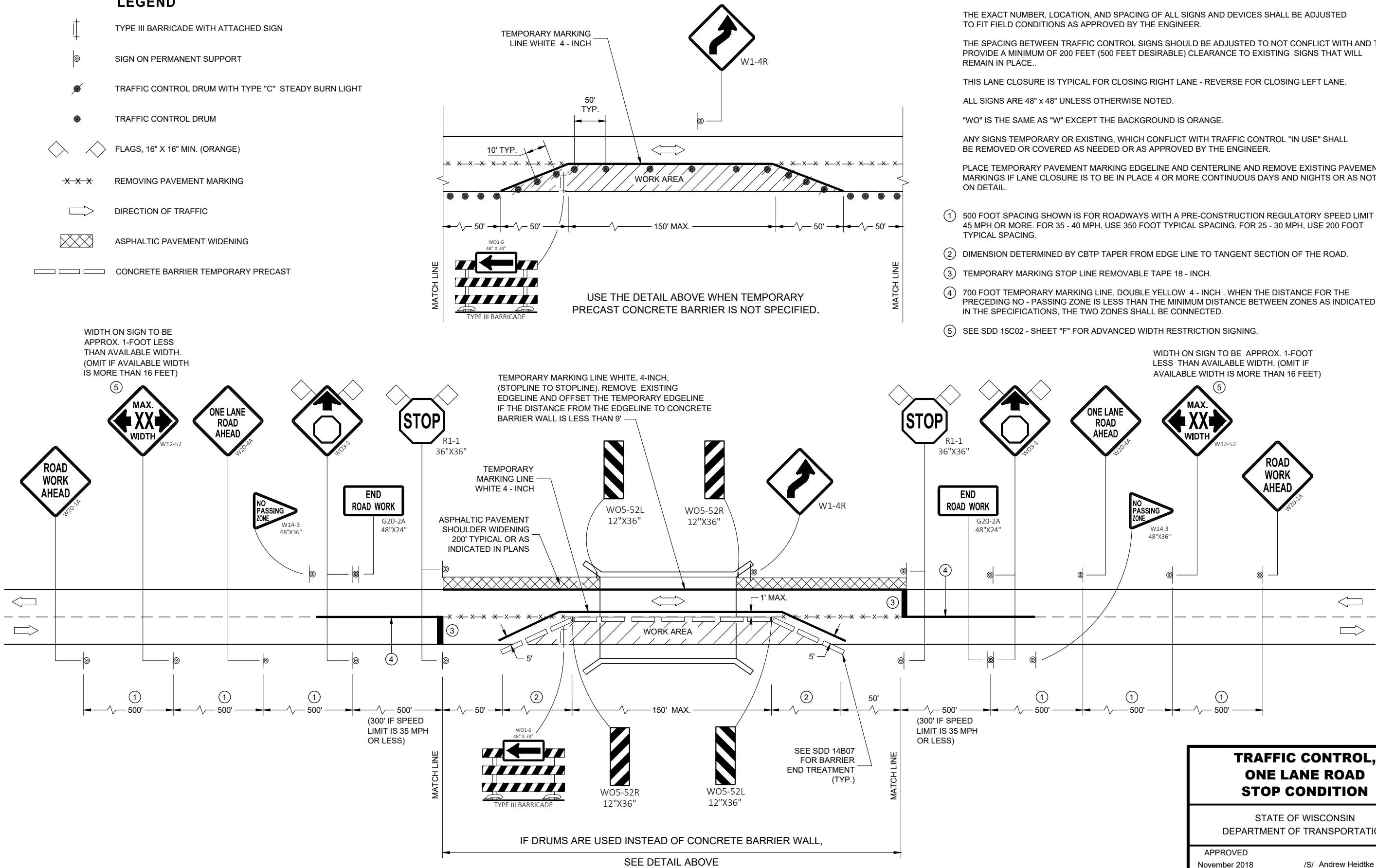
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLAGS, 16" X 16" MIN. (ORANGE)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- ASPHALTIC PAVEMENT WIDENING
- CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..
- THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.
- ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.
- "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.
- PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.
- 500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.
 - DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.
 - TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18 - INCH.
 - 700 FOOT TEMPORARY MARKING LINE, DOUBLE YELLOW 4 - INCH . WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
 - SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.







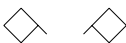





**TRAFFIC CONTROL,
ONE LANE ROAD
STOP CONDITION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

- 
- TYPE III BARRICADE WITH ATTACHED SIGN
- 
- SIGN ON PERMANENT SUPPORT
- 
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- 
- TRAFFIC CONTROL DRUM
- 
- FLAGS, 16" X 16" MIN. (ORANGE)
- 
- REMOVING PAVEMENT MARKING
- 
- DIRECTION OF TRAFFIC
- 
- ASPHALTIC PAVEMENT WIDENING
- 
- CONCRETE BARRIER TEMPORARY PRECAST
- 
- TEMPORARY SIGNAL. SEE SDD 09G02 FOR EXACT PLACEMENT

WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)

- ①

500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.
- ②

USE 300 FOOT SPACING IF THE PRE - CONSTRUCTION REGULATORY SPEED IS 35 MPH OR LESS.
- ③

DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.
- ④

TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18 - INCH.
- ⑤

700 FOOT TEMPORARY MARKING DOUBLE YELLOW LINE 4 - INCH. WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
- ⑥

SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..

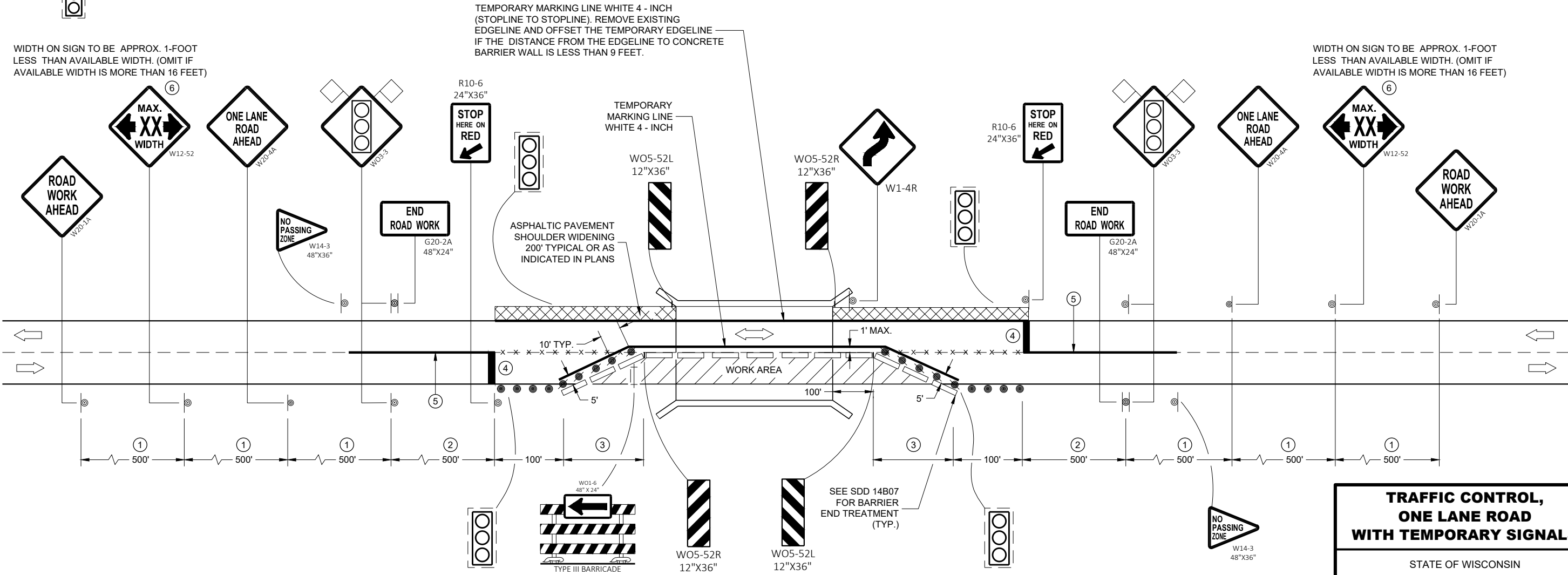
THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.



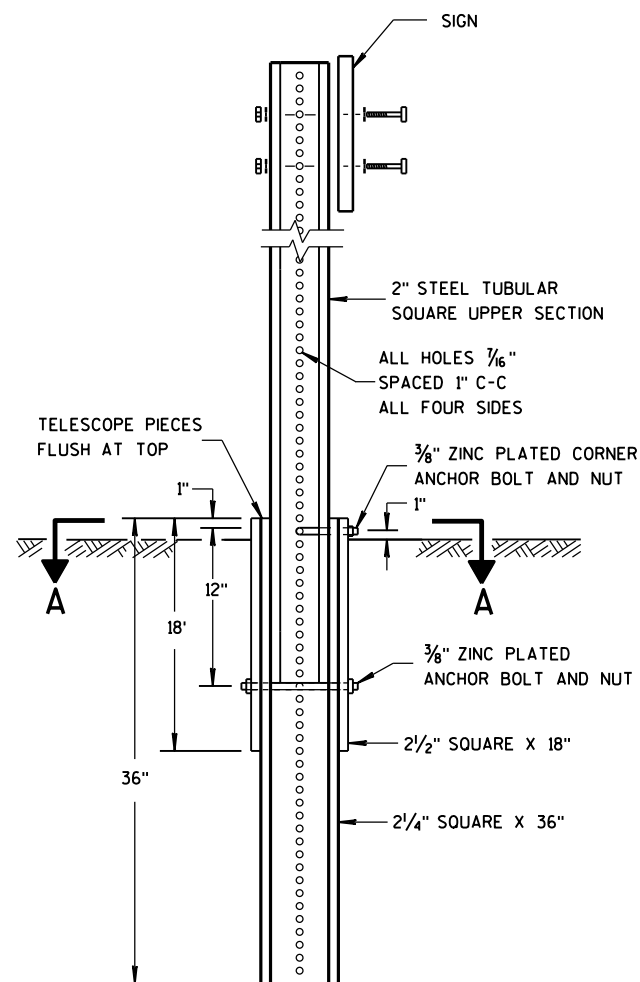
**TRAFFIC CONTROL,
ONE LANE ROAD
WITH TEMPORARY SIGNALS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA



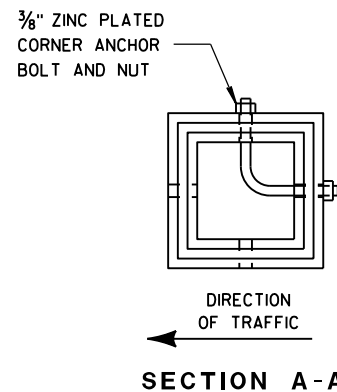
DETAIL OF TUBULAR
STEEL SIGN POST

TUBULAR STEEL POSTS

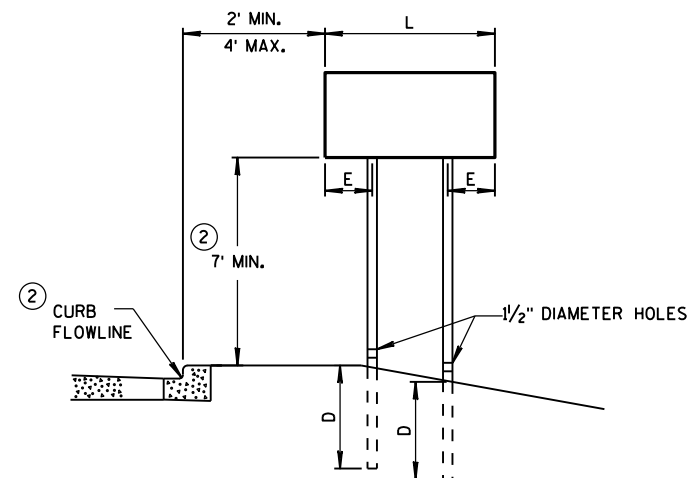
AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL
BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED
ON TUBULAR STEEL POSTS.



SECTION A-A

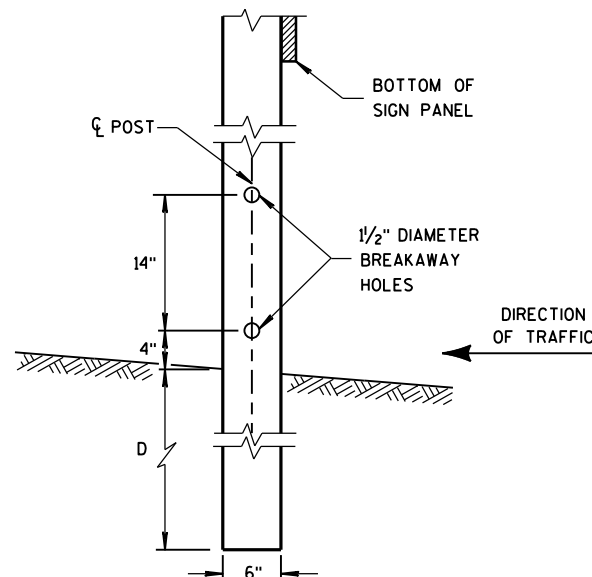


URBAN AREA

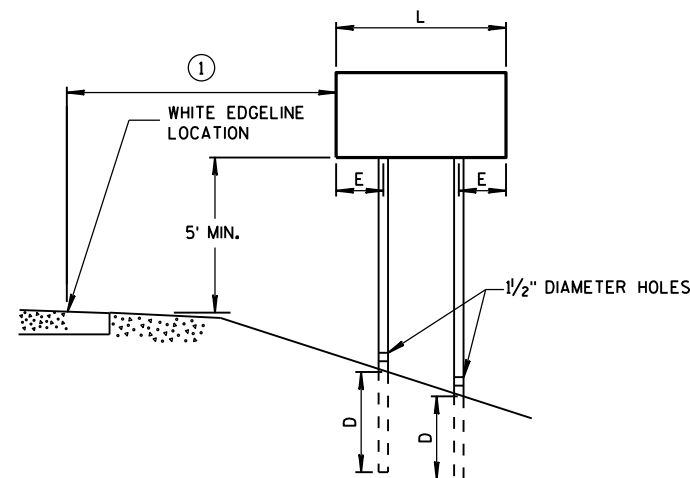
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST
EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



4 "x6 " WOOD POST
MODIFICATION



RURAL AREA

4 " X 6 " WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL
SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

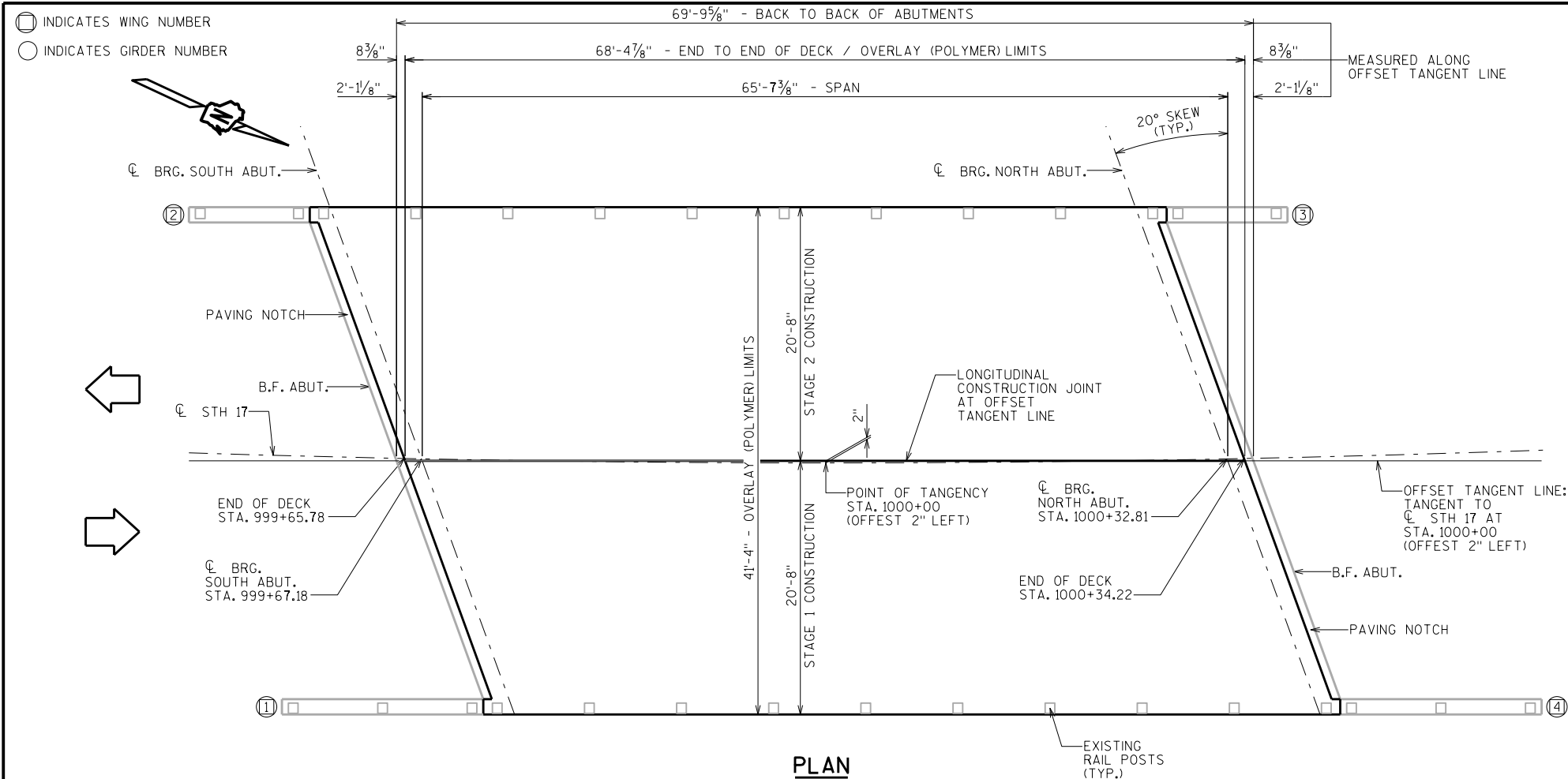
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" X 3"
 - MACHINE BOLTS - 5/16" X 6-1/2" OR 7" LENGTH W/ NUTS

- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS
 - RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 - 1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



PLAN

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-24
OPERATING RATING: HS-43
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

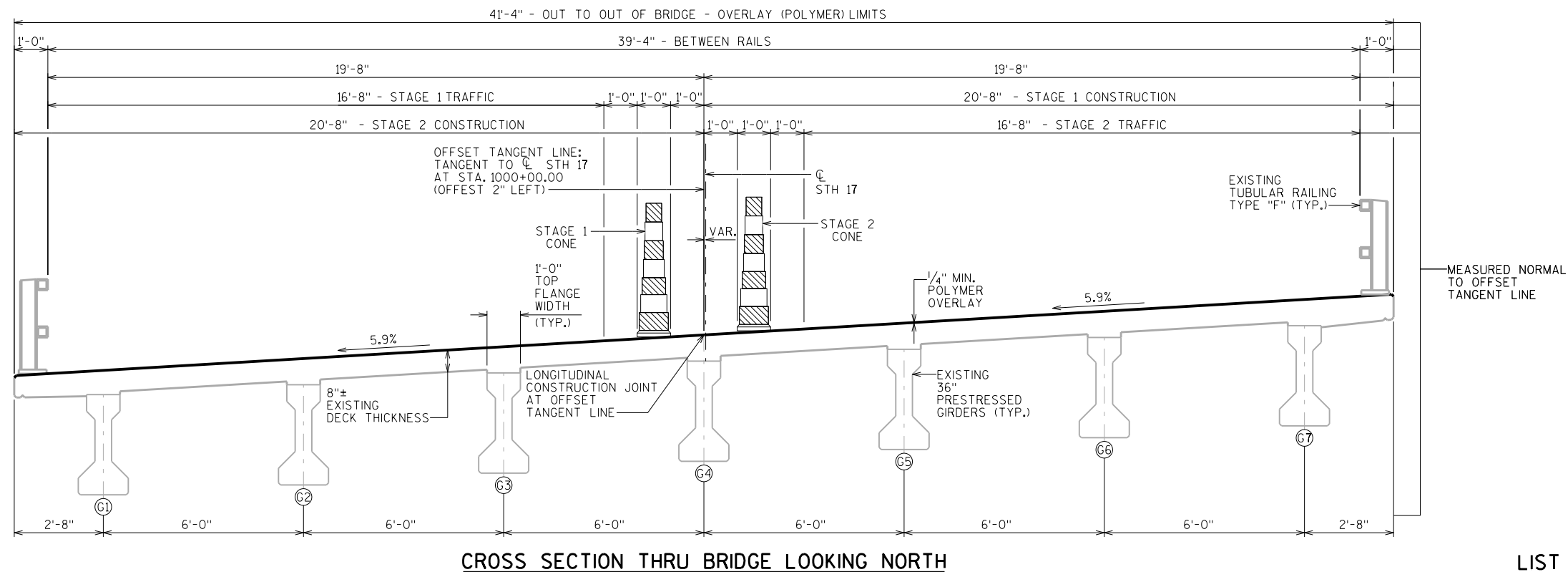
TRAFFIC VOLUME

STH 17
A.D.T. = 1,800 (2016)
R.D.S. = 60 M.P.H.



TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	315



CROSS SECTION THRU BRIDGE LOOKING NORTH

LIST OF DRAWINGS

1. POLYMER OVERLAY

STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
ACCEPTED <i>William C. Diehl</i> 2/15/19 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-35-101			
STH 17 OVER PRAIRIE RIVER			
COUNTY	LINCOLN	TOWN	RUSSELL
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH PLANS CK'D. JJS
POLYMER OVERLAY			SHEET 1 OF 1

I.D. 1009-47-31A

DATE: OCT. 2018

SCALE = 6.00

INVENTORY RATING: HS-16
OPERATING RATING: HS-28
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 230 KIPS

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

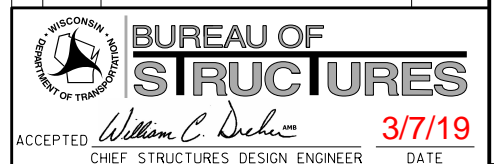
A.D.T. = 3,500 (2016)
R.D.S. = 60 M.P.H.



BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	264

JOHN SENDOR	(608) 266-5163
AARON BONK	(608) 261-0261

NO.	DATE	REVISION	BY
-----	------	----------	----



STRUCTURE B-37-06

STH. 153 OVER BRANCH BIG EAU PLEINE RIVER

COUNTY	MARATHON	TOWN	STRATFORD
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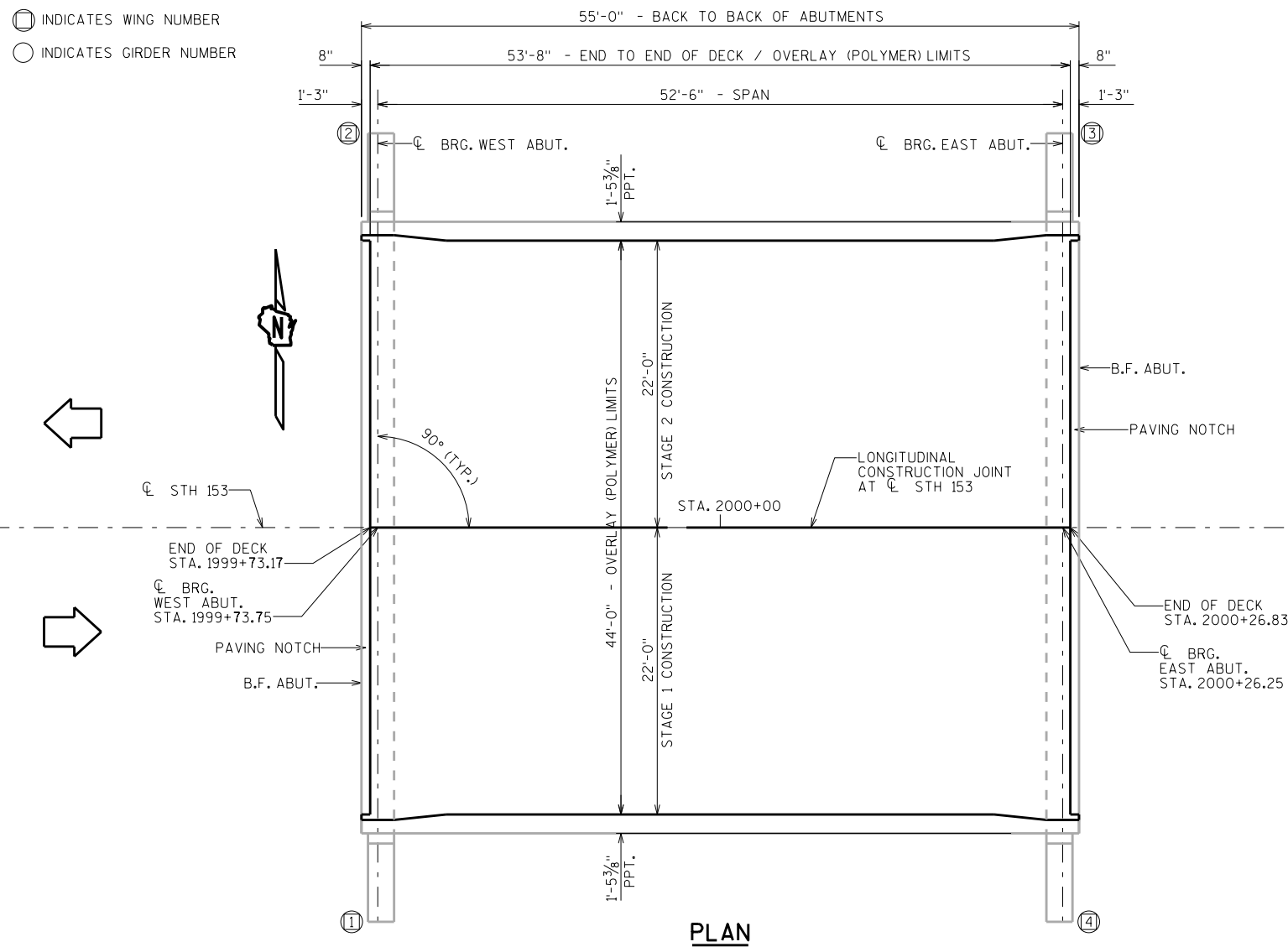
DESIGN SPEC.	
REHABILITATION	N/A

DESIGNED BY JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH	PLANS CK'D. JJS
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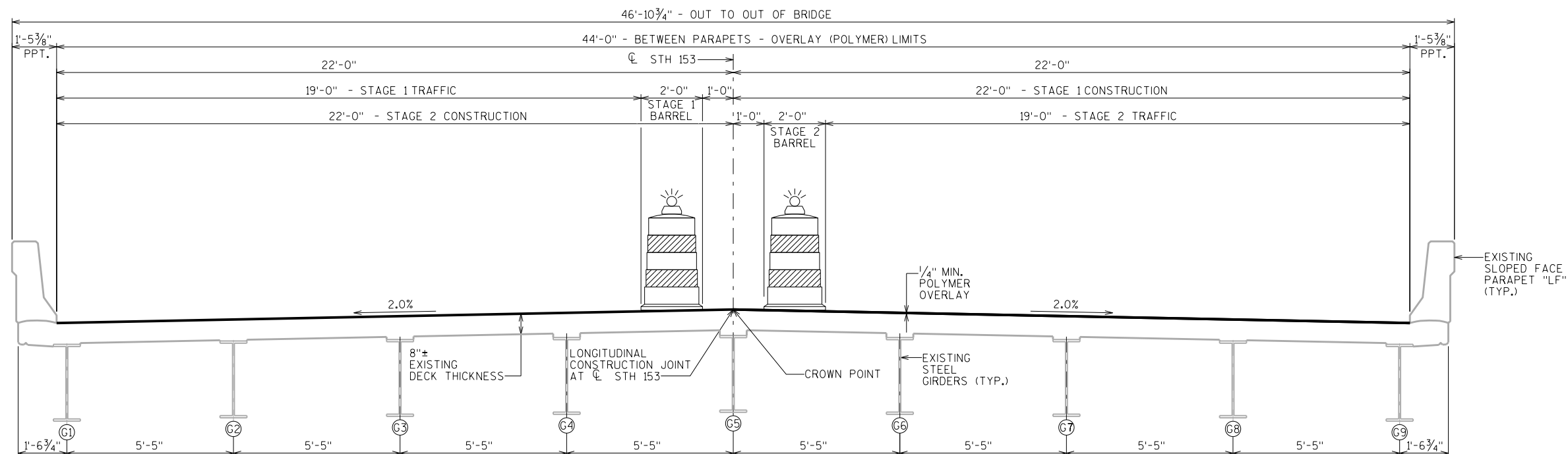
POLYMER OVERLAY	SHEET 1 OF 1

--	--

DATE: OCT. 2018

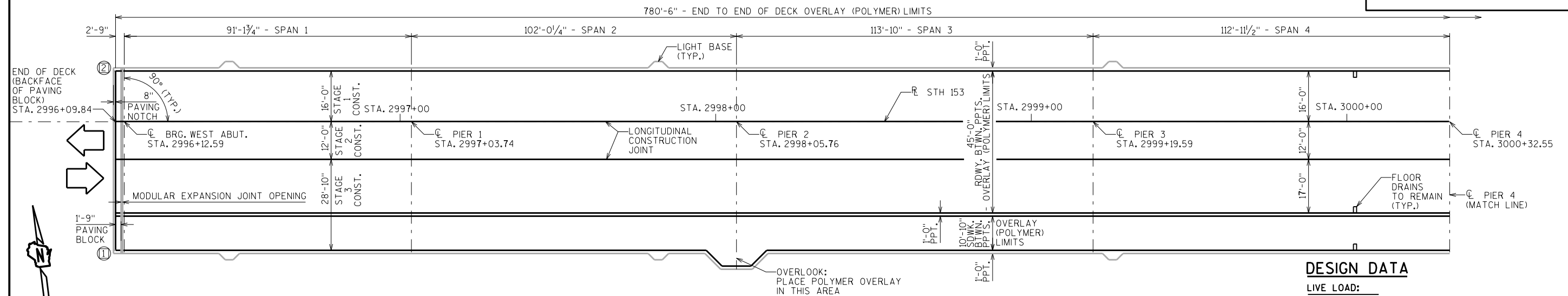


CROSS SECTION THRU BRIDGE LOOKING EAST



LIST OF DRAWINGS

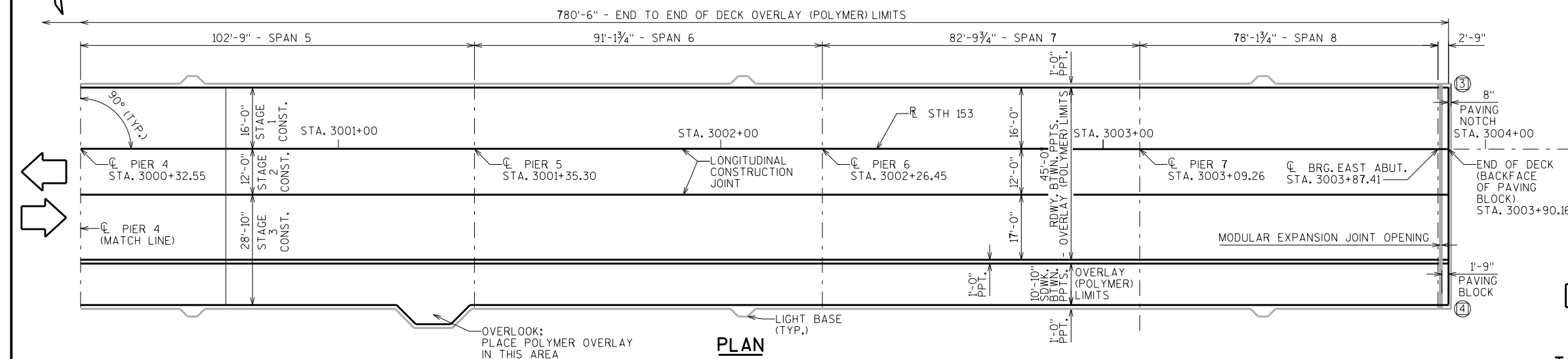
1. POLYMER OVERLAY



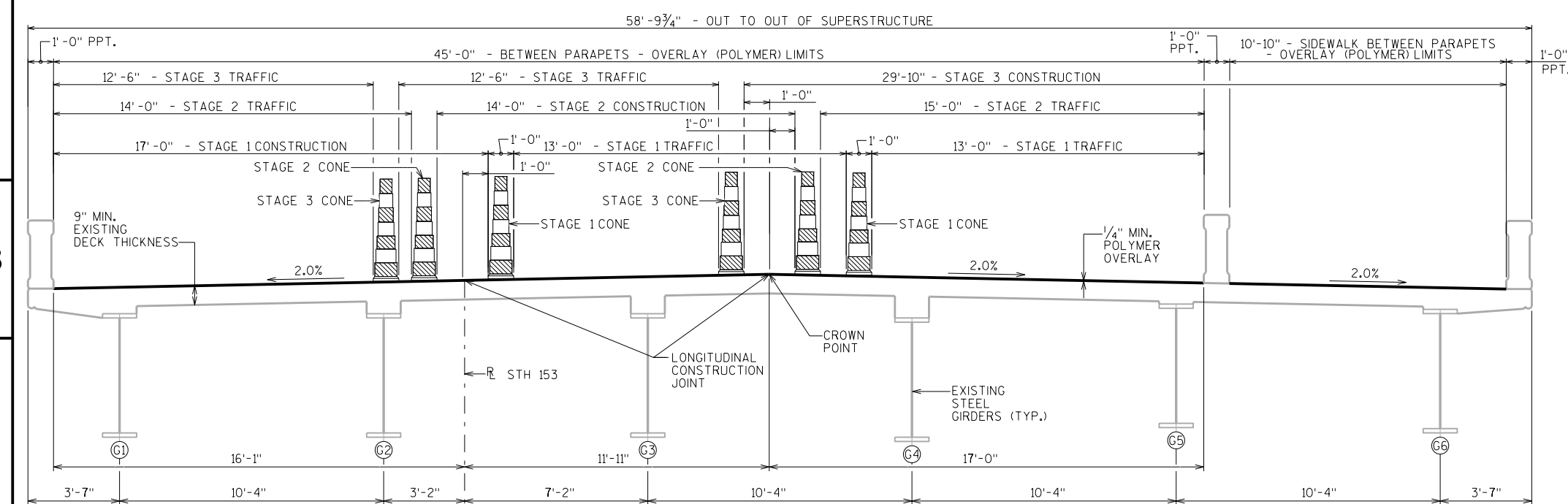
INVENTORY RATING: HS-16
OPERATING RATING: HS-28
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 200 KIPS

DECK SURFACE PREPARATION IS INCLUDED
IN THE BID ITEM "POLYMER OVERLAY".

A.D.T. = 12,500 (2016)
R.D.S. = 30 M.P.H.




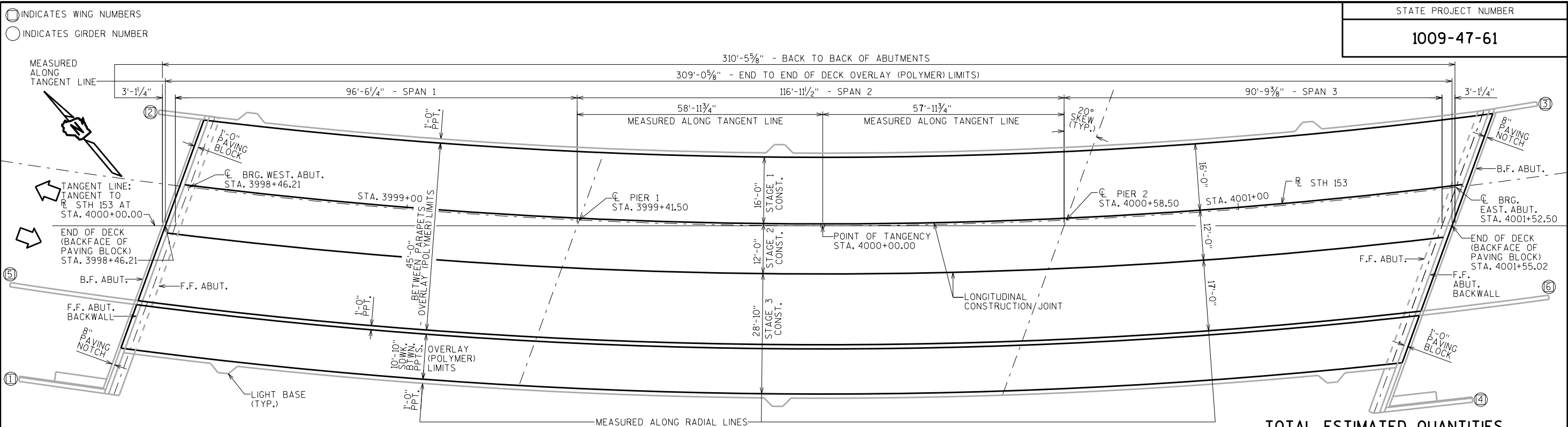
BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	4,860



1. POLYMER OVERLAY

JOHN SENDOR	(608) 266-5163
AARON BONK	(608) 261-0261

NO.		DATE		REVISION		BY	
				BUREAU OF STRUCTURES			
ACCEPTED		<i>William C. Diehn</i> ^{AME}				3/7/19	
		CHIEF STRUCTURES DESIGN ENGINEER				DATE	
STRUCTURE B-37-89							
STH 153 OVER WISCONSIN RIVER/RAILROAD/DEPOT ST							
COUNTY				MARATHON		CITY	
						MOSINEE	
DESIGN SPEC. REHABILITATION N/A							
DESIGNED BY		JJS		DESIGNED CK'D. RATINGS		DRAWN BY	
						JPH	
						PLANS CK'D. JJS	
POLYMER OVERLAY						SHEET 1 OF 1	



TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	1,920

CURVE DATA

R STH 153
P.I. STA. = STA. 16+64.59
P.C. STA. = STA. 11+62.98
P.T. STA. = STA. 21+17.36
DELTA = 43°-35'-00"
T = 501.61'
R = 1254.65'
L = 954.38'
D = 04°-34'-00"

TRAFFIC VOLUME

STH 153
A.D.T. = 11,700 (2013)
R.D.S. = 30 M.P.H.

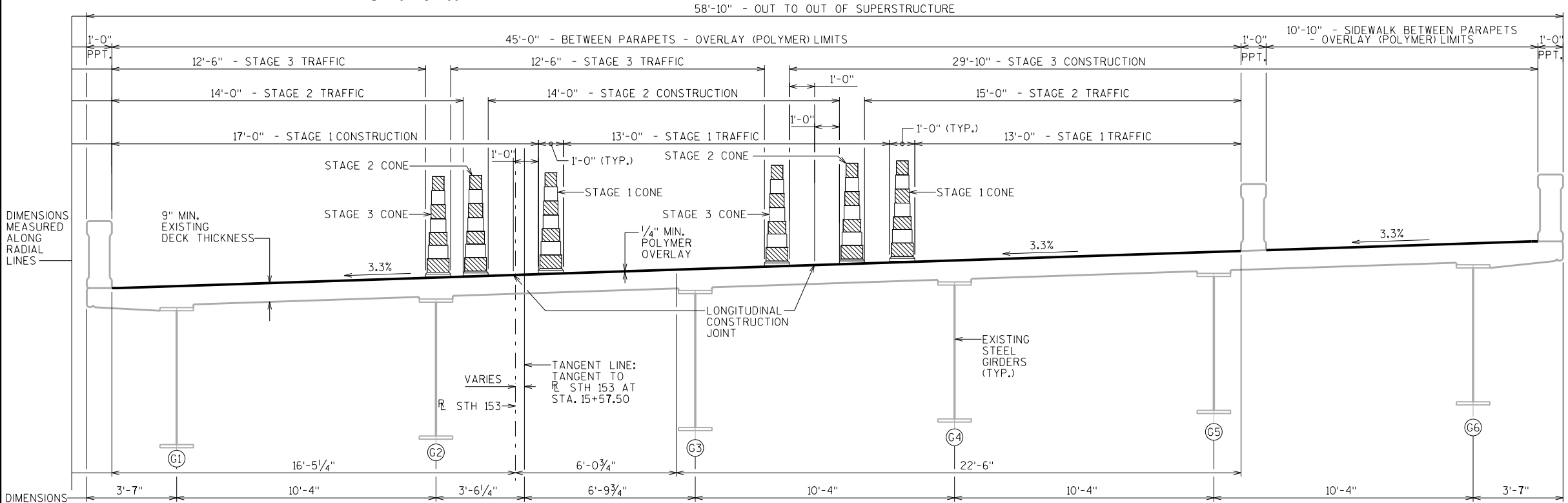
DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-15
OPERATING RATING: HS-26
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 180 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261



TYPICAL CROSS SECTION THRU BRIDGE LOOKING EAST

LIST OF DRAWINGS

1. POLYMER OVERLAY

POLYMER OVERLAY

SHEET 1 OF 1

INDICATES WING NUMBER

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-27
OPERATING RATING: HS-45
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

STATE PROJECT NUMBER

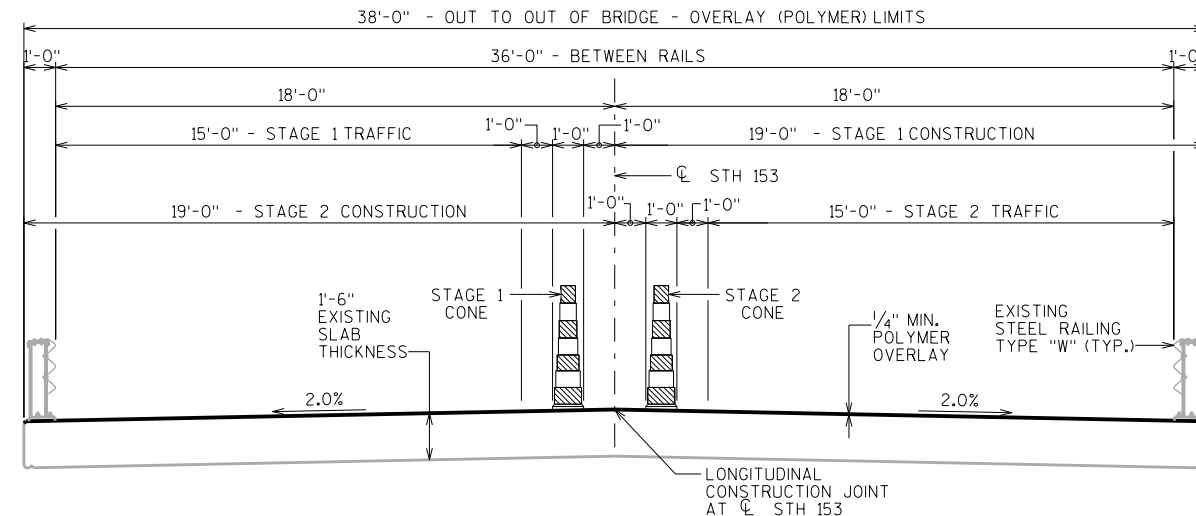
1009-47-61

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 153
A.D.T. = 1,300 (2016)
R.D.S. = 60 M.P.H.



CROSS SECTION THRU BRIDGE LOOKING EAST

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	132

STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
ACCEPTED <i>William C. Decker</i> 3/7/19 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-37-305			
STH 153 OVER LITTLE EAU CLAIRE RIVER			
COUNTY	MARATHON	TOWN	REID
DESIGN SPEC. REHABILITATION	N/A		
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH PLANS CK'D. JJS
POLYMER OVERLAY			SHEET 1 OF 1

LIST OF DRAWINGS

1. POLYMER OVERLAY

I.D. 1009-47-31E

DATE: OCT. 2018

SCALE = 3.00

8

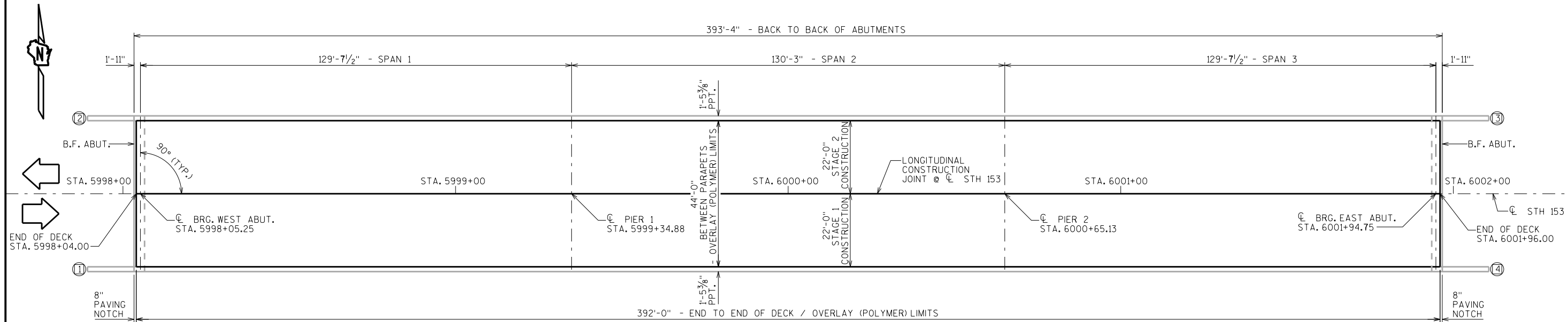
8

PLAN

INDICATES WING NUMBERS

STATE PROJECT NUMBER

1009-47-61



PLAN

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	1,917

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-24
OPERATING RATING: HS-47
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES


DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 153
A.D.T. = 3,000 (2016)
R.D.S. = 60 M.P.H.



STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
 BUREAU OF STRUCTURES			
ACCEPTED <i>William C. Dehner</i>		3/7/19	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-37-306			
STH 153 OVER BIG EAU PLEINE RIVER			
COUNTY	MARATHON	TOWN	CLEVELAND
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	BY JPH
PLANS CK'D. JJS		SHEET 1 OF 1	
POLYMER OVERLAY			

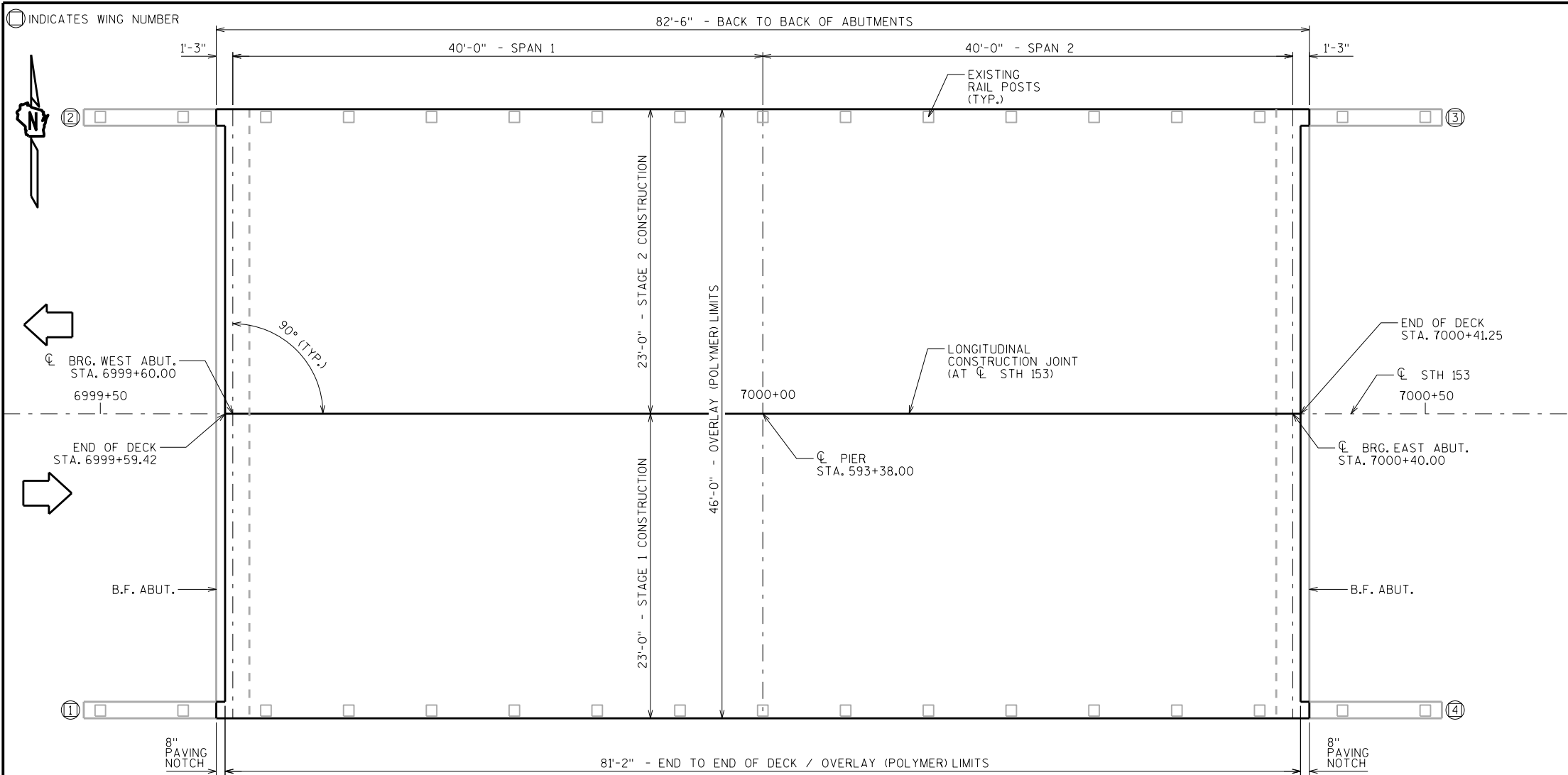
LIST OF DRAWINGS

1. POLYMER OVERLAY

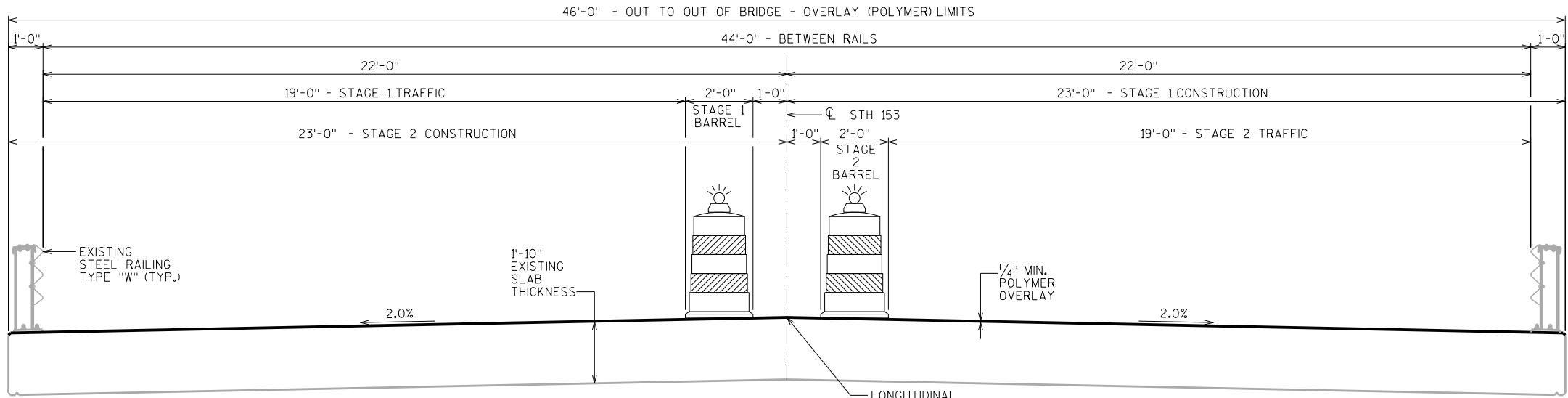
I.D. 1009-47-31F

DATE: OCT. 2018

SCALE = 15.00



PLAN



CROSS SECTION THRU BRIDGE LOOKING EAST

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-22
OPERATING RATING: HS-37
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 153
A.D.T. = 3,900 (2004)
R.D.S. = 60 M.P.H.




TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	414

STRUCTURE DESIGN CONTACTS:

JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
<div><div><div><div>BUREAU OF</div><div>STRUCTURES</div></div></div><div>ACCEPTED <i>William C. Decker</i> 3/7/19 CHIEF STRUCTURES DESIGN ENGINEER DATE</div></div>			
STRUCTURE B-37-308			
STH 153 OVER FREEMAN CREEK			
COUNTY	MARATHON	TOWN	EMMET
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH PLANS CK'D. JJS
POLYMER OVERLAY			SHEET 1 OF 1

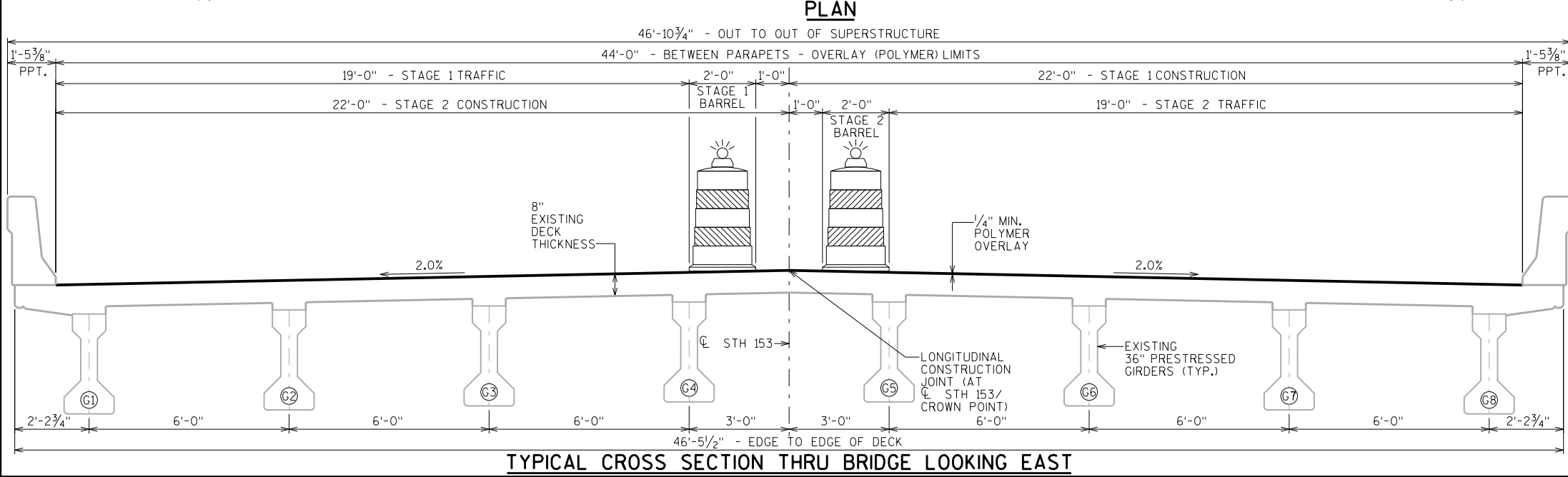
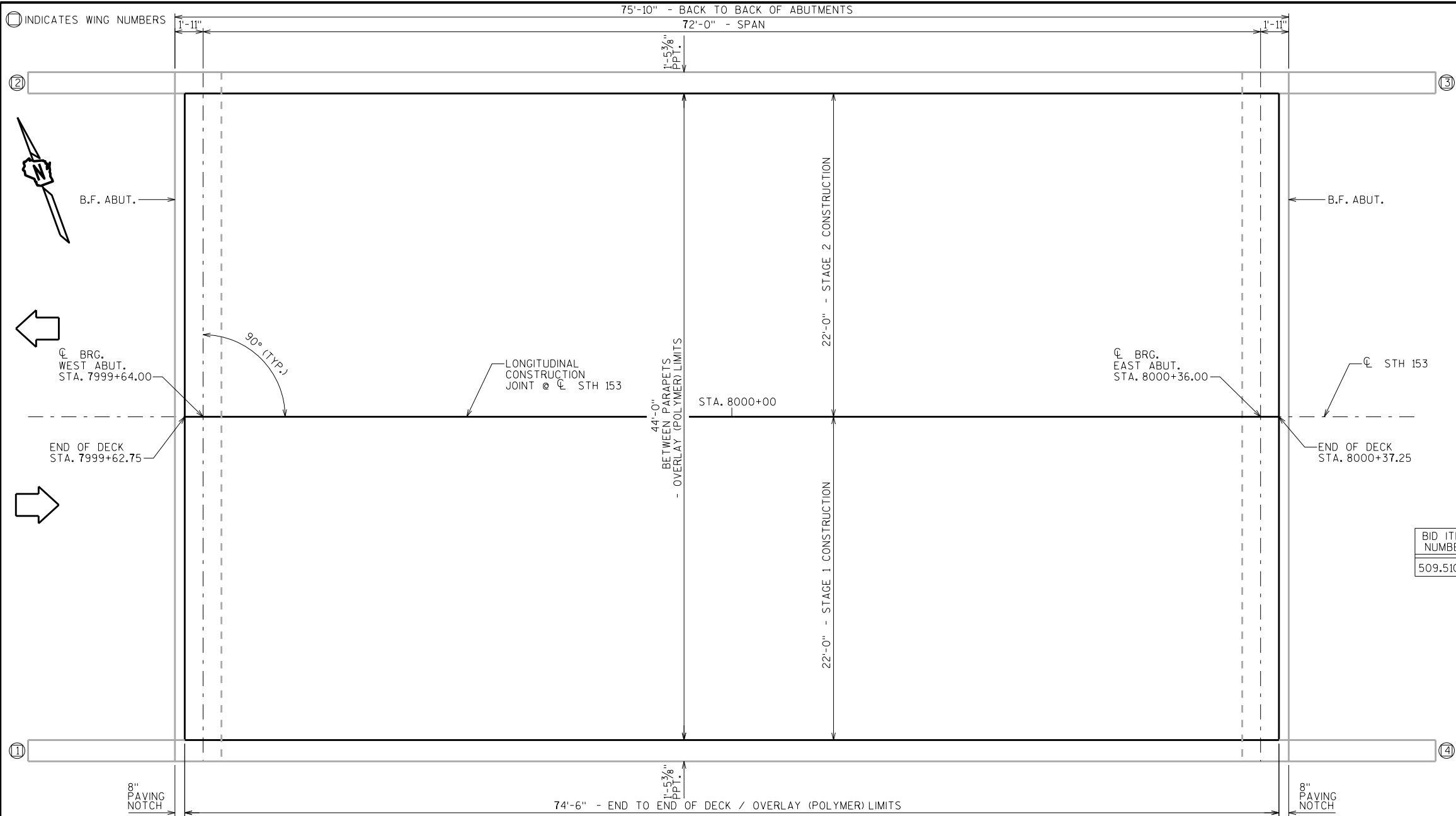
LIST OF DRAWINGS

1. POLYMER OVERLAY

I.D. 1009-47-31G

DATE: OCT. 2018

SCALE = 5.00



STATE PROJECT NUMBER

1009-47-61

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-22
OPERATING RATING: HS-47
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 153
A.D.T. = 3,000 (2016)
R.D.S. = 60 M.P.H.



TOTAL ESTIMATED QUANTITIES


BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	365

STRUCTURE DESIGN CONTACTS:

JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

LIST OF DRAWINGS

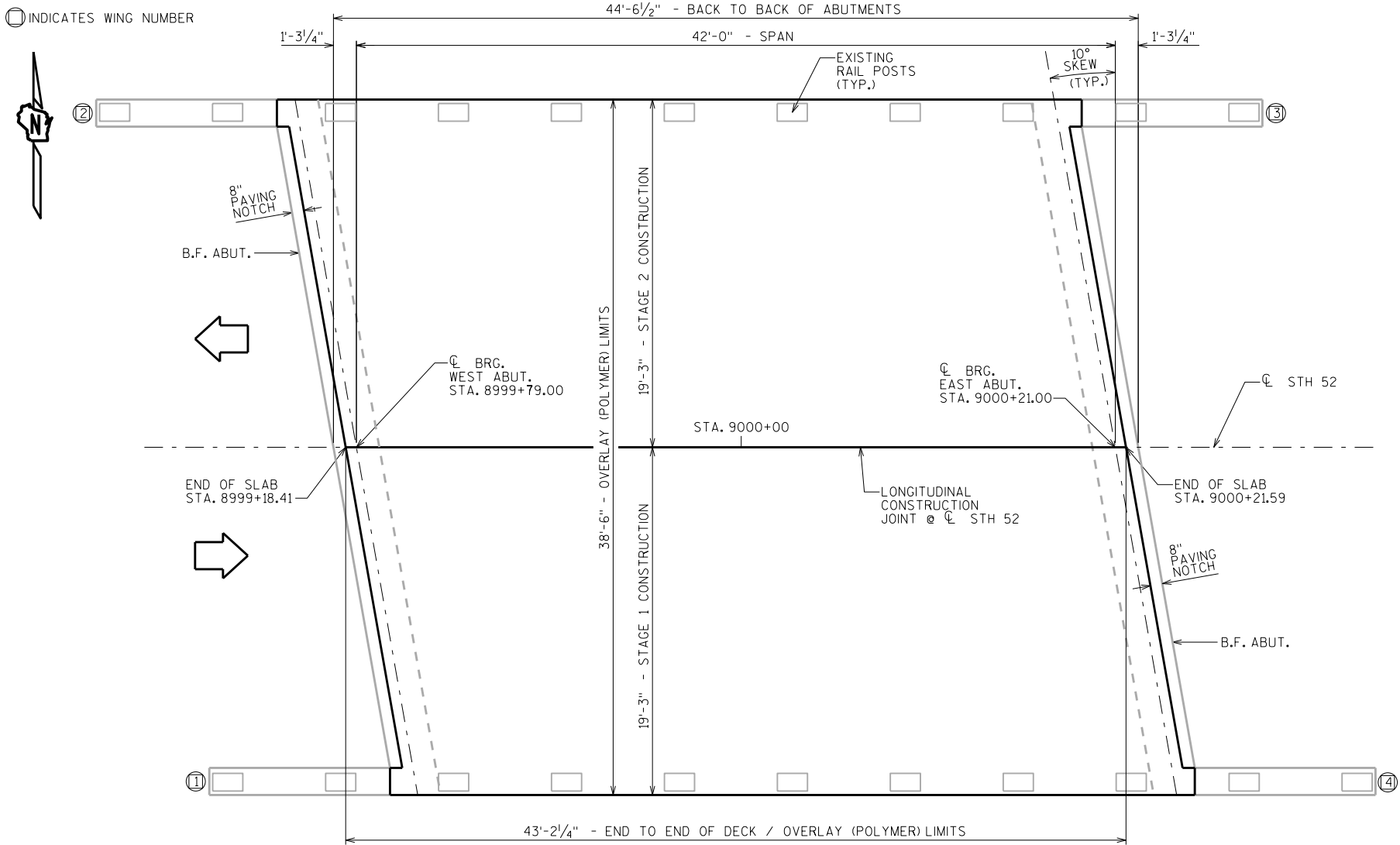
1. POLYMER OVERLAY

NO.	DATE	REVISION	BY
<div><div><div><div><div>BUREAU OF</div><div>STRUCTURES</div></div><div>ACCEPTED <i>William C. Decker</i> 3/7/19 CHIEF STRUCTURES DESIGN ENGINEER DATE</div></div></div><div><div>STRUCTURE B-37-315</div><div>STH 153 OVER HOG CREEK</div><div>COUNTY MARATHON TOWN MOSINEE</div><div>DESIGN SPEC. REHABILITATION N/A</div><div>DESIGNED BY JJS DESIGNED CK'D. RATINGS N/A DRAWN BY JPH PLANS CK'D. JJS</div><div>POLYMER OVERLAY</div></div><div>SHEET 1 OF 1</div></div>			

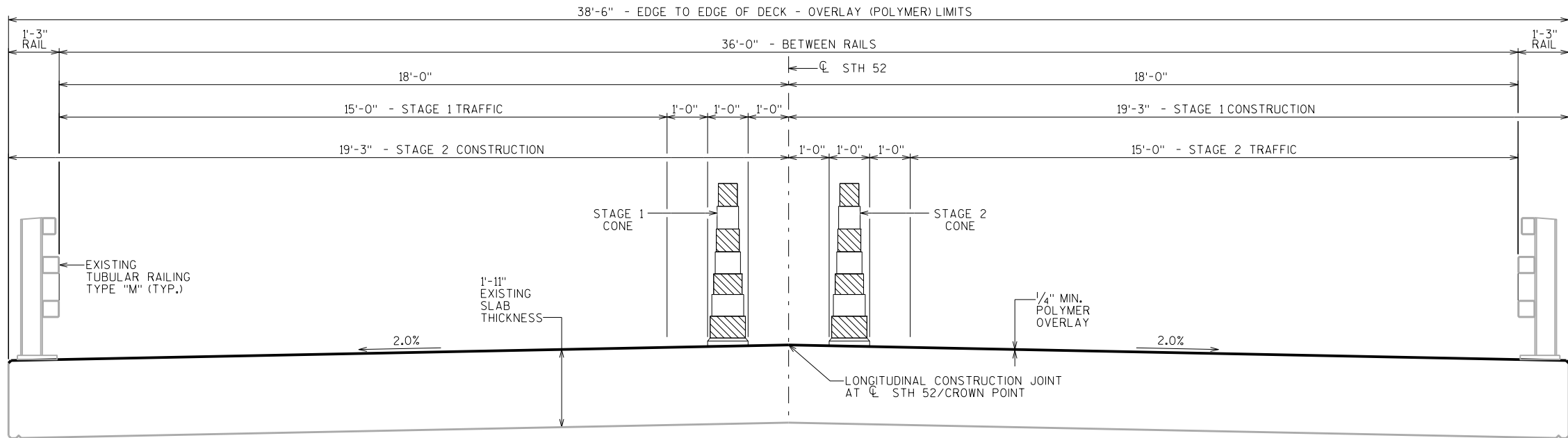
I.D. 1009-47-31H

DATE: OCT. 2018

SCALE = 4.00



PLAN



CROSS SECTION THRU BRIDGE LOOKING EAST

LIST OF DRAWINGS

1. POLYMER OVERLAY

STATE PROJECT NUMBER

1009-47-61

DESIGN DATA

LIVE LOAD:

INVENTORY RATING: HS-24
OPERATING RATING: HS-41
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 52

A.D.T. = 2,300 (2013)
R.D.S. = 60 M.P.H.




TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	183

STRUCTURE DESIGN CONTACTS:

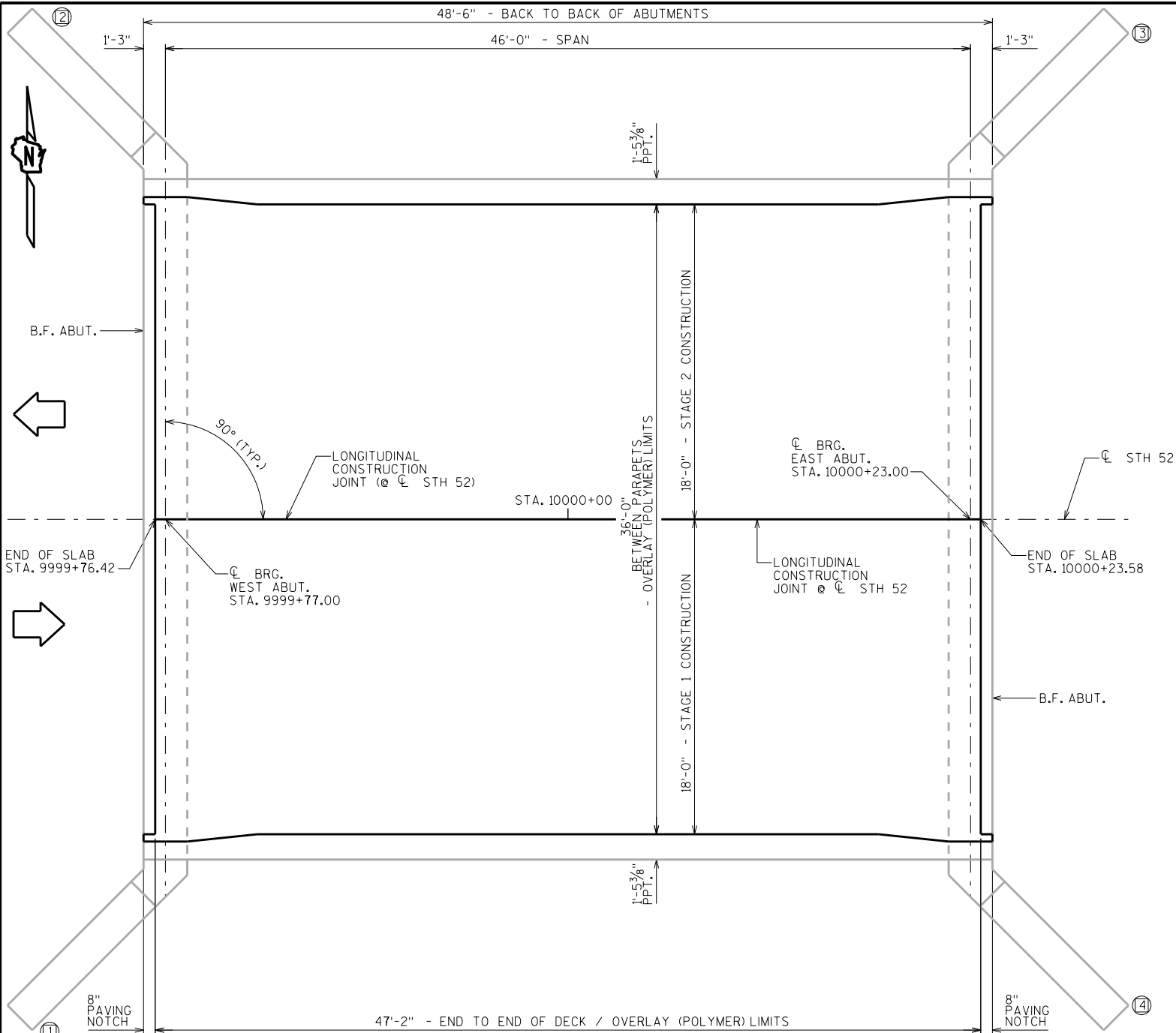
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
 BUREAU OF STRUCTURES			
ACCEPTED <i>William C. Decker</i>		3/7/19	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-37-318			
STH 52 OVER WEST BRANCH BIG SANDY CREEK			
COUNTY	MARATHON	TOWN	HEWITT
DESIGN SPEC. REHABILITATION	N/A		
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	N/A
DRAWN BY	JPH	PLANS CK'D.	JJS
POLYMER OVERLAY			SHEET 1 OF 1

I.D. 1009-47-31I

DATE: OCT. 2018

SCALE = 4.00



Ⓢ INDICATES WING NUMBER

STATE PROJECT NUMBER
1009-47-61

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-21
OPERATING RATING: HS-35
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 52
A.D.T. = 2,300 (2013)
R.D.S. = 60 M.P.H.



TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	190

PLAN

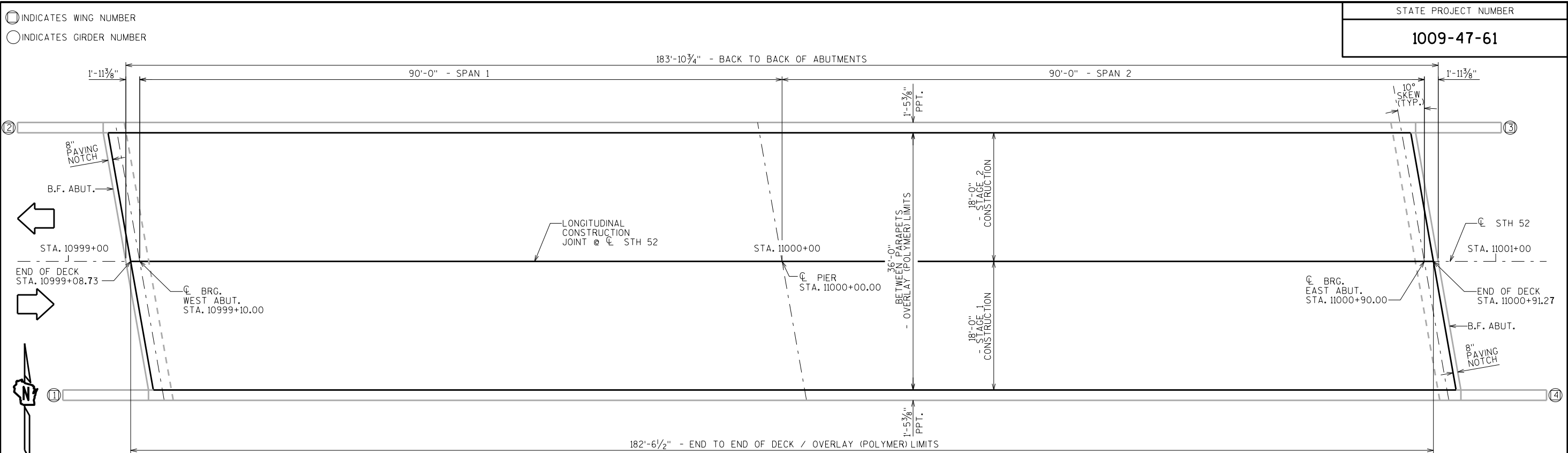
STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
<div><div></div><div>BUREAU OF STRUCTURES</div></div>			
ACCEPTED		3/7/19	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-37-319			
STH 52 OVER BIG SANDY CREEK			
COUNTY	MARATHON	TOWN	HEWITT
DESIGN SPEC.	N/A		
REHABILITATION	N/A		
DESIGNED BY	DESIGNED CK'D. RATINGS	DRAWN BY	PLANS CK'D.
JJS		JPH	JJS
POLYMER OVERLAY			SHEET 1 OF 1

LIST OF DRAWINGS

1. POLYMER OVERLAY

SCALE = 4.00



TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	731

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-25
OPERATING RATING: HS-42
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.

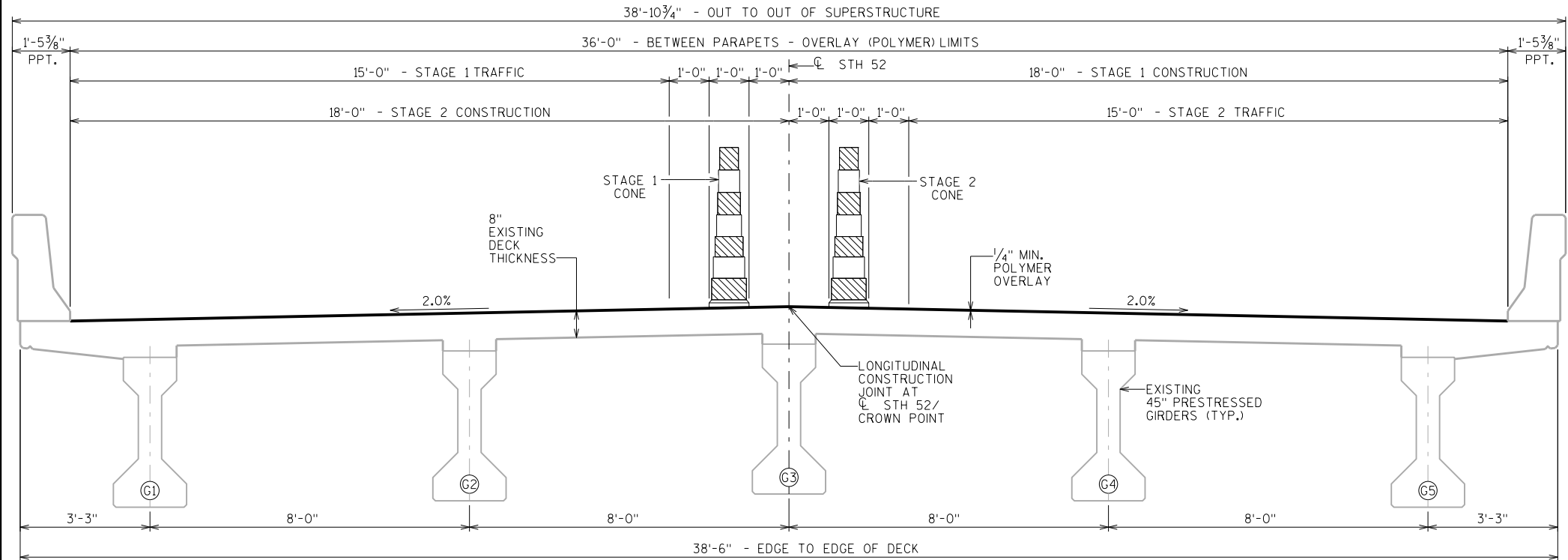
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 52
A.D.T. = 1,845 (2013)
R.D.S. = 60 M.P.H.

STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261



TYPICAL CROSS SECTION THRU BRIDGE LOOKING EAST

LIST OF DRAWINGS

1. POLYMER OVERLAY

NO.	DATE	REVISION	BY
BUREAU OF STRUCTURES			
ACCEPTED <i>William C. Decker</i> 3/7/19 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-37-320			
STH 52 OVER EAU CLAIRE RIVER			
COUNTY	MARATHON	TOWN	HARRISON
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH
POLYMER OVERLAY			PLANS CK'D. JJS
SHEET 1 OF 1			

I.D. 1009-47-31K

DATE: OCT. 2018

SCALE = 7.00



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>

WISCONSIN
PROJECT ID: 1009-47-62
COUNTY: SHAWANO

MAY 2019

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plan
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plans
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 40



DESIGN DESIGNATION*

A.A.D.T.	VAR.	=	STH 22 VAR.
A.A.D.T.	2015	=	STH 47 VAR
A.A.D.T.	2015	=	STH 156 1,300
D.H.V.		=	
D.D.		=	
T.		=	
DESIGN SPEED		=	
ESALS		=	

*SEE GENERAL NOTES SHEET FOR
STRUCTURE AADT SUMMARY TABLE

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

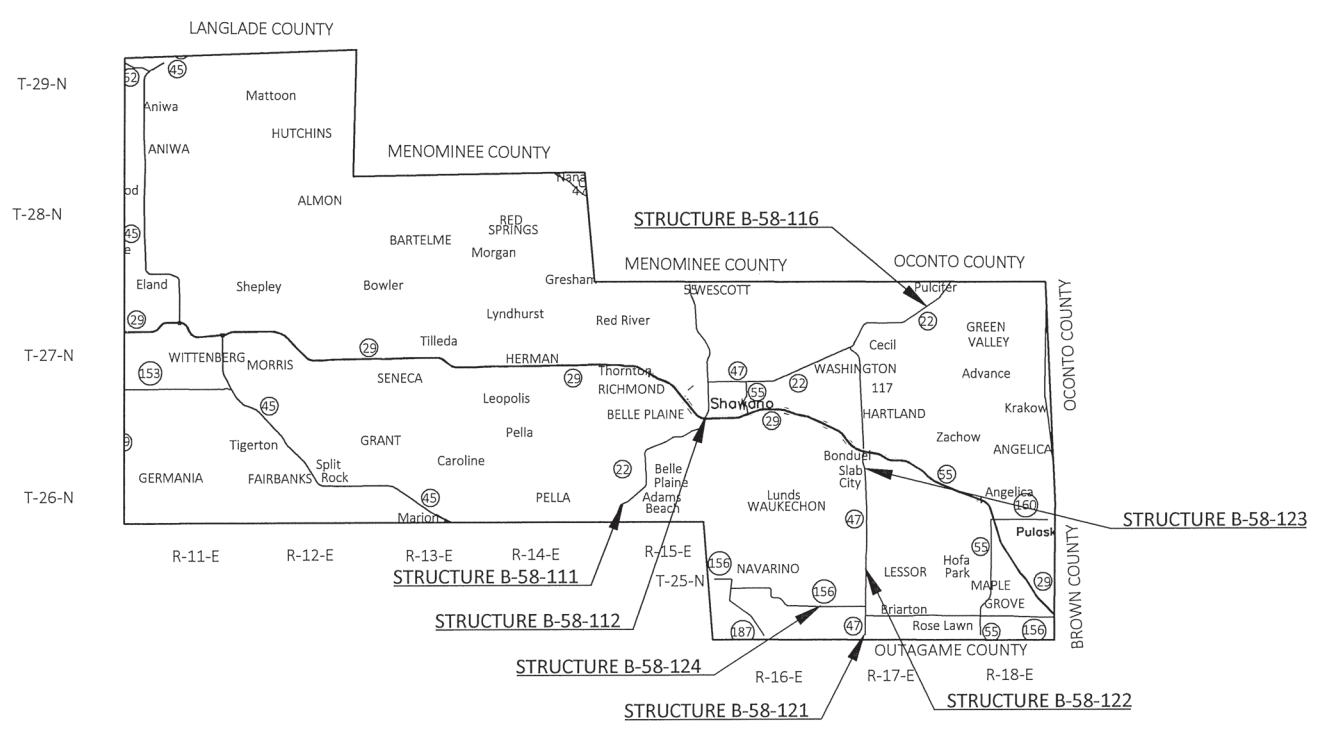
PLAN OF PROPOSED IMPROVEMENT

REGIONWIDE BRIDGE POLYMER OVERLAYS

SHAWANO COUNTY BRIDGE POLYMER OVERLAYS

VAR HWY
SHAWANO COUNTY

STATE PROJECT NUMBER
1009-47-62



TOTAL NET LENGTH OF CENTERLINE = STH 22: 0 MILES, STH 47: 0 MILES, STH 156: 0 MILES

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1009-47-62	WISC 2019302	1

ORIGINAL PLANS PREPARED BY

SEH

DANIEL A. PENZKOVER
30248
RICE LAKE
WI

DATE: 1/24/19

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	N/A
Designer	SEH
Project Manager	WENDY ARNESON
Regional Examiner	CHERYL SIMON
Regional Supervisor	MICHAEL KRETSCHMER

APPROVED FOR THE DEPARTMENT

DATE: 1/25/19

Wendy Arneson
(Signature)

E

STRUCTURES INCLUDED IN PROJECT


STH 22 OVER THE EMBARRASS RIVER (B-58-111)
STH 22 OVER THE WOLF RIVER (B-58-112)
STH 22 OVER THE OCONTO RIVER (B-58-116)
STH 47 OVER AN UNNAMED BRANCH OF HERMAN CREEK (B-58-121)
STH 47 OVER THE MINK CREEK (B-58-122)
STH 47 OVER THE SLAB CITY CREEK (B-58-123)
STH 156 OVER THE SHIOC RIVER (B-58-124)

GENERAL NOTES

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.



Dial  or (800)242-8511
www.DiggersHotline.com

SIGNAL TIMING INFORMATION

STRUCTURE AADT SUMMARY TABLE		
STRUCTURE I.D.	YEAR	AADT
B-58-111	2015	4,600
B-58-112	2015	7,700
B-58-116	2012	2,900
B-58-121	2015	4,900
B-58-122	2015	4,900
B-58-123	2015	4,900
B-58-124	2015	1,300

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		EB	WB	EB	WB	EB	WB	EB	WB
B-58-0111	ALL	2.8	2.8	15.3	15.3	26.9	26.9	45.0	45.0

STRUCTURE #	STAGE #	YELLOW				ALL RED				MIN GREEN				MAX GREEN			
		Φ1	Φ2	Φ4	Φ8	Φ1	Φ2	Φ4	Φ8	Φ1	Φ2	Φ4	Φ8	Φ1	Φ2	Φ4	Φ8
		NB	SB	EB	WB	NB	SB	EB	WB	NB	SB	EB	WB	NB	SB	EB	WB
B-58-0112	ALL	2.8	2.8	3.6	3.6	22.9	22.9	2.8	2.8	15.0	15.0	10.0	10.0	30.0	30.0	15.0	15.0

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		EB	WB	EB	WB	EB	WB	EB	WB
B-58-0116	ALL	2.8	2.8	17.4	17.4	24.8	24.8	45.0	45.0

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		NB	SB	NB	SB	NB	SB	NB	SB
B-58-0121	ALL	2.8	2.8	10.8	10.8	26.4	26.4	40.0	40.0

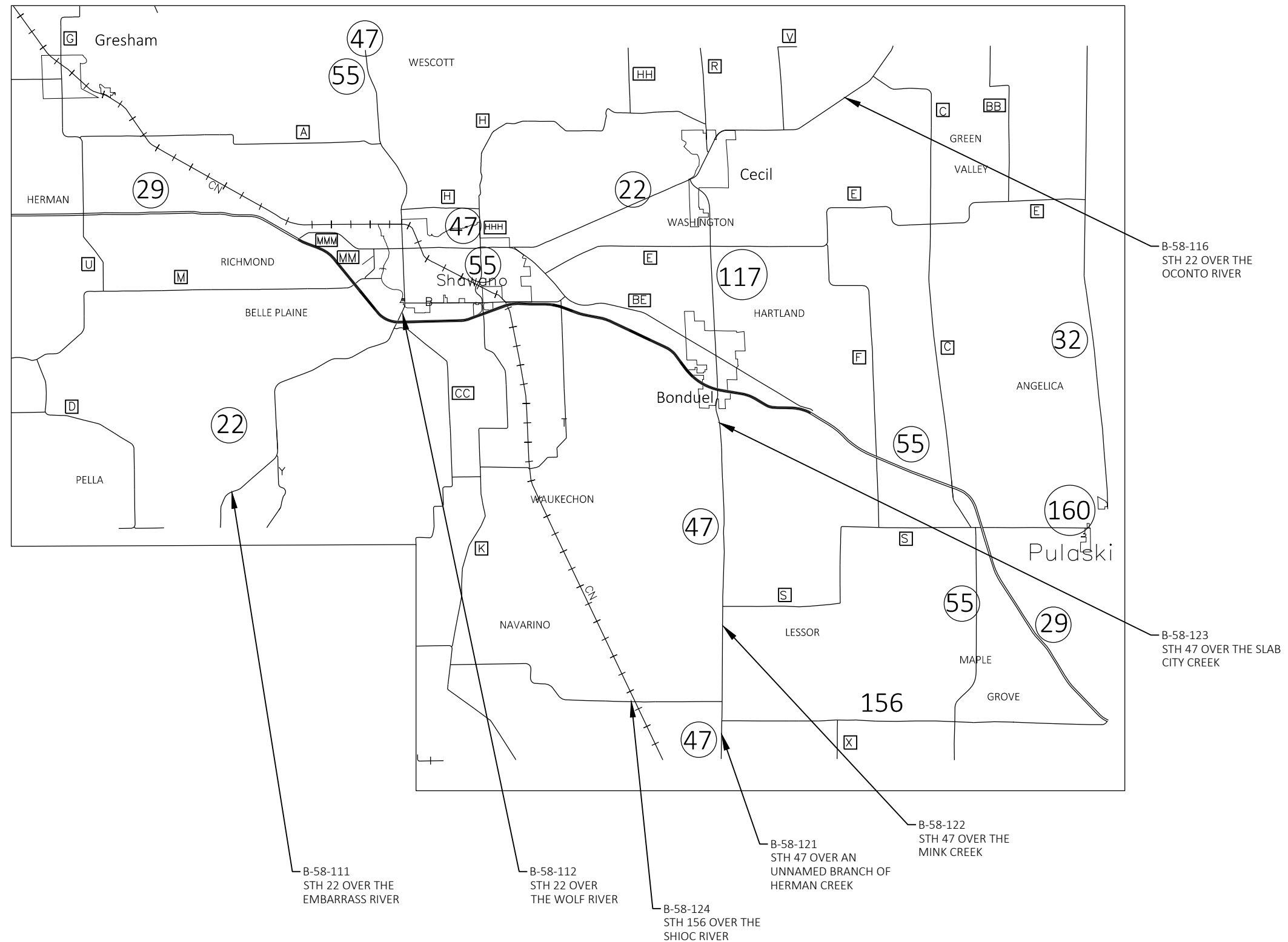
STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		NB	SB	NB	SB	NB	SB	NB	SB
B-58-0122	ALL	2.8	2.8	11.3	11.3	25.9	25.9	40.0	40.0

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		NB	SB	NB	SB	NB	SB	NB	SB
B-58-0123	ALL	2.8	2.8	11.4	11.4	25.8	25.8	40.0	40.0

STRUCTURE #	STAGE #	YELLOW		ALL RED		GREEN		TOTAL SPLIT	
		Φ1	Φ2	Φ1	Φ2	Φ1	Φ2	Φ1	Φ2
		EB	WB	EB	WB	EB	WB	EB	WB
B-58-0124	ALL	2.8	2.8	9.7	9.7	22.5	22.5	35.0	35.0

WDNR CONTACT
2984 SHAWANO AVENUE
GREEN BAY, WI 54313
TELEPHONE: 920.412.0165
ATTENTION: JAMES DOPLERASKI
EMAIL: JAMES.DOPERALSKI@WISCONSIN.GOV

PROJECT OVERVIEW - STH 22, STH 47 AND STH 156 BRIDGES



PROJECT NO: 1009-47-62

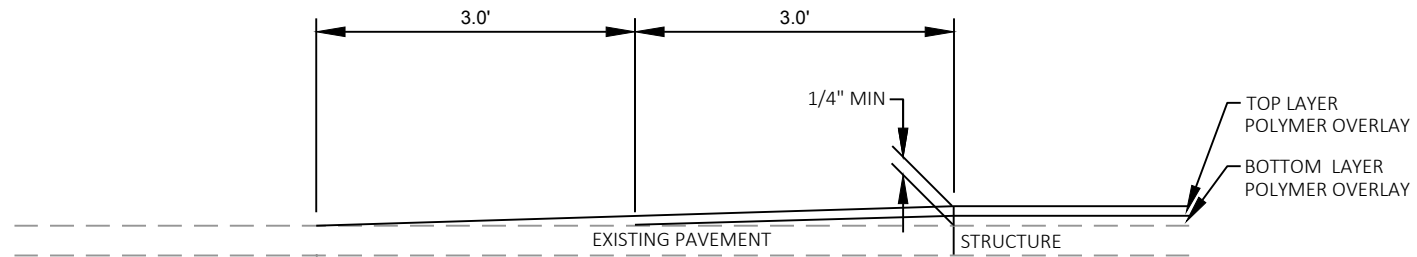
HWY: VAR HWY

COUNTY: SHAWANO

PROJECT OVERVIEW

SHEET

E



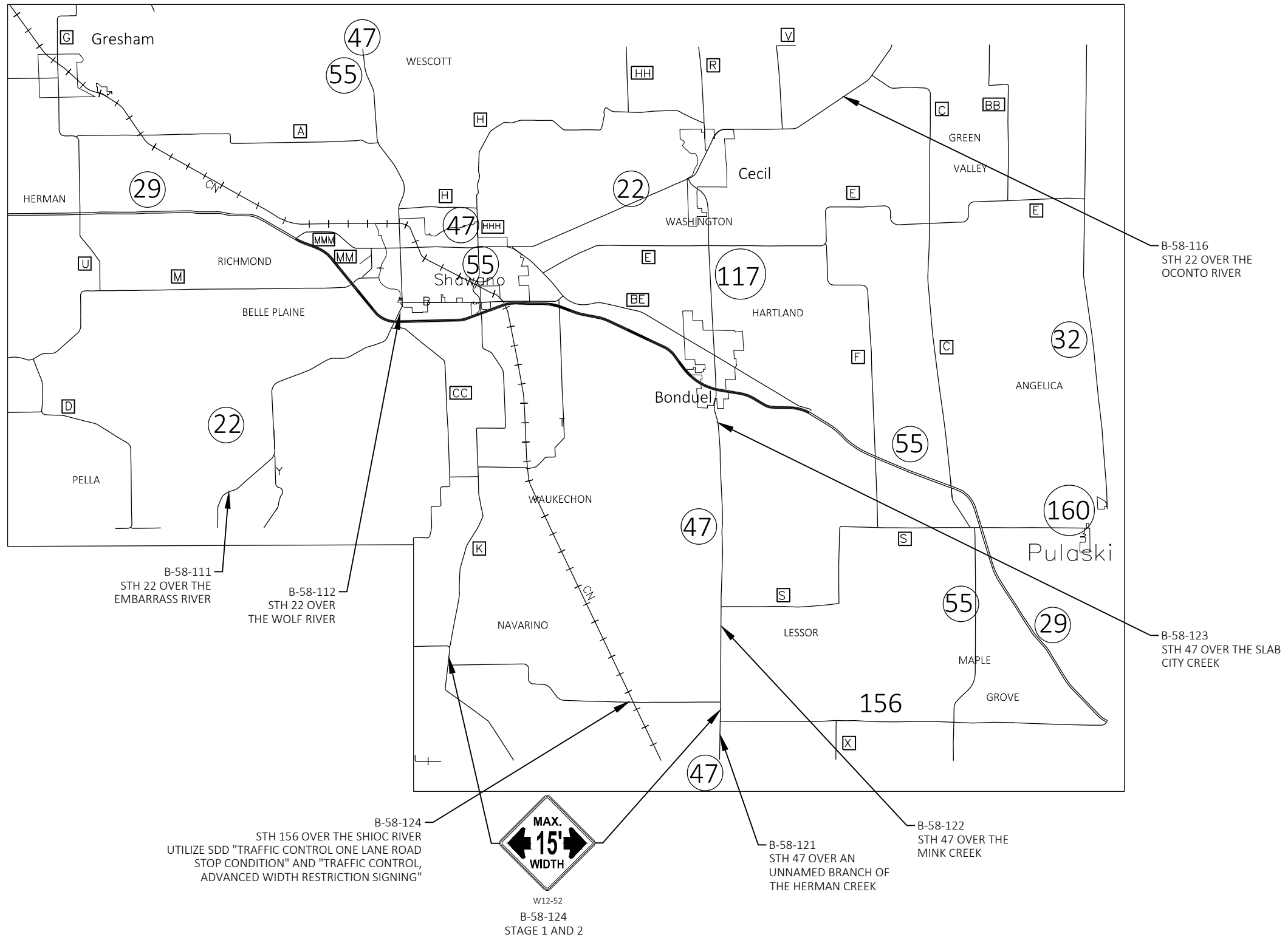
POLYMER OVERLAY TRANSITIONAL AREA DETAIL

NOTE: POLYMER OVERLAY TO BE PLACED FULL WIDTH COVERING PAVEMENT AND SHOULDERS.

MASK OFF EXISTING STRUCTURE JOINT AS DIRECTED BY THE ENGINEER.

POLYMER TRANSITIONAL AREA INCIDENTAL TO "POLYMER OVERLAY" BID ITEM ON STRUCTURE PLANS.

TRAFFIC CONTROL PROJECT OVERVIEW - STH 22, STH 47 AND STH 156 BRIDGES



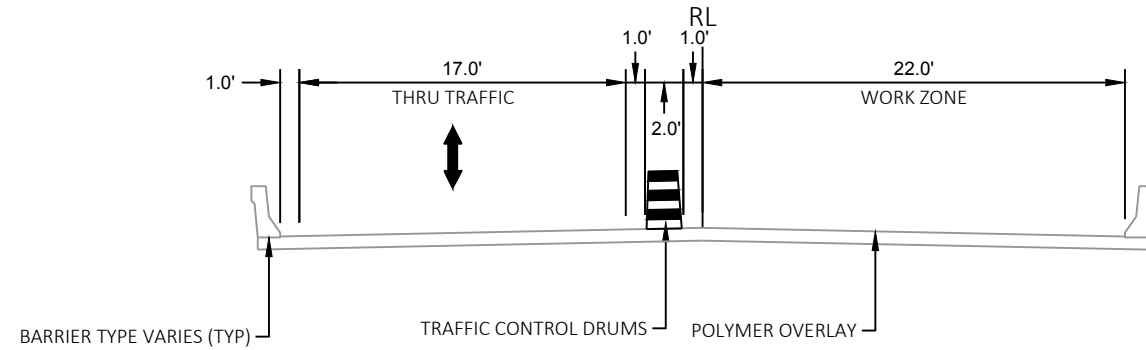
NOTE:
UTILIZE SDD "TRAFFIC CONTROL, ONE LANE ROAD
WITH TEMPORARY SIGNALS" AND "BRIDGE
TEMPORARY TRAFFIC SIGNAL INSTALLATION" AT
ALL POLYMER OVERLAY LOCATIONS.*

* UNLESS NOTED OTHERWISE.

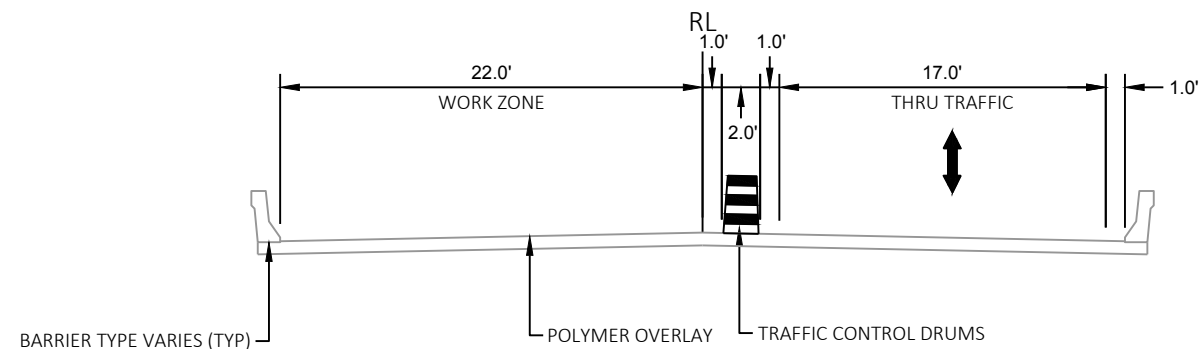
B-58-124
STH 156 OVER THE SHIOCK RIVER
UTILIZE SDD "TRAFFIC CONTROL ONE LANE ROAD
STOP CONDITION" AND "TRAFFIC CONTROL,
ADVANCED WIDTH RESTRICTION SIGNING"



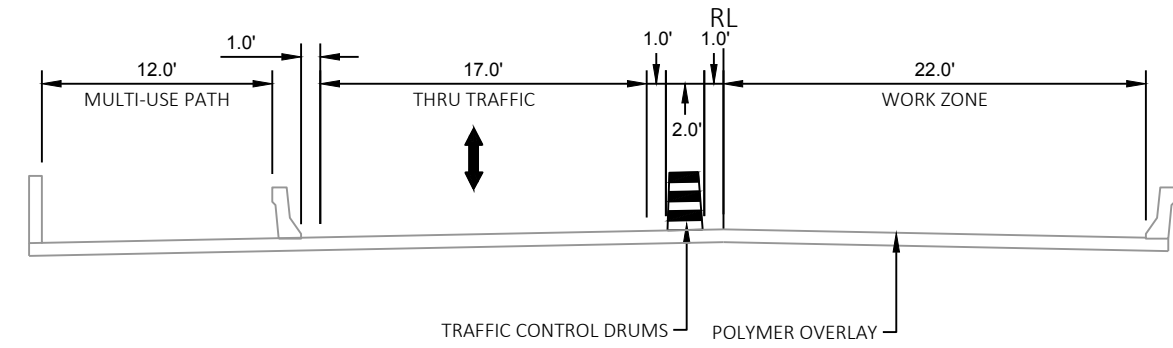
W12-52
B-58-124
STAGE 1 AND 2



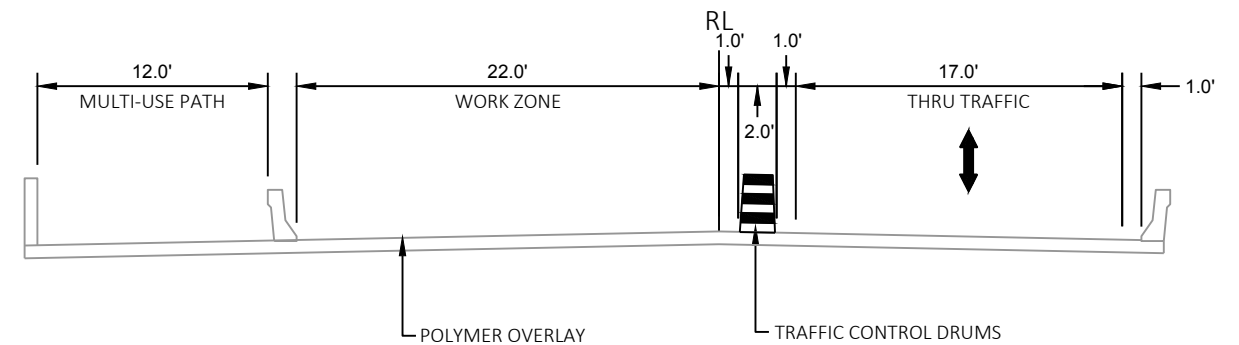
±44' CLEAR WIDTH TYPICAL SECTION - STAGE 1
B-58-111, B-58-116, B-58-121, B-58-122, B-58-123



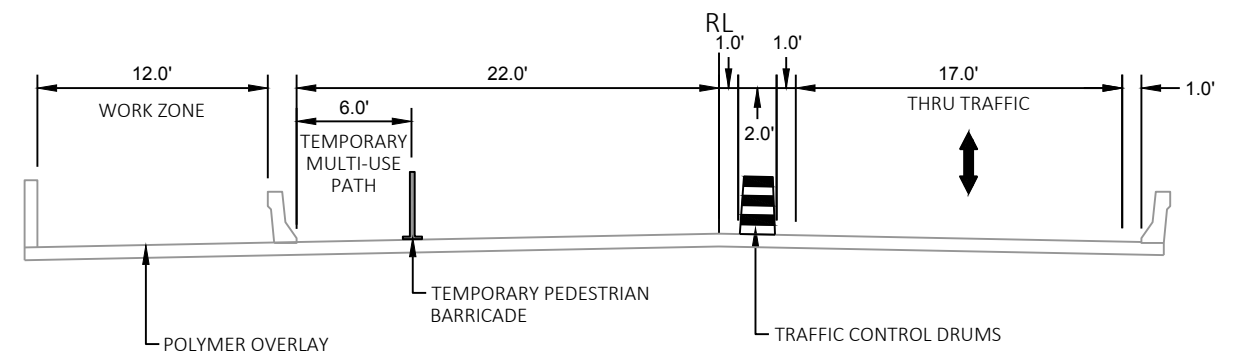
±44' CLEAR WIDTH TYPICAL SECTION - STAGE 2
B-58-111, B-58-116, B-58-121, B-58-122, B-58-123



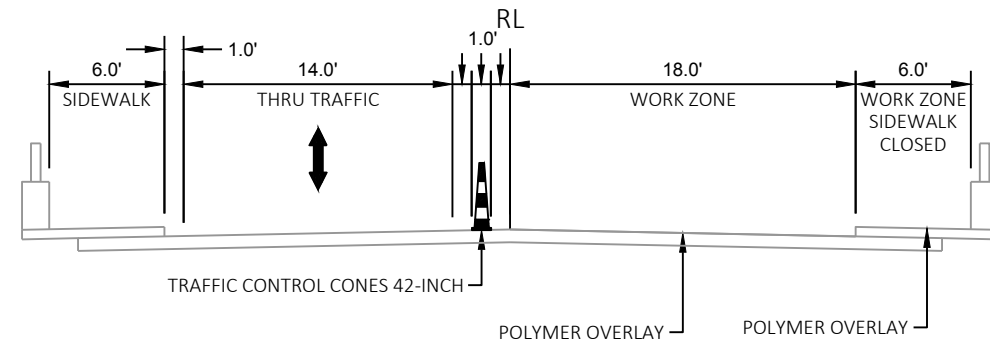
±56' TYPICAL SECTION - STAGE 1
B-58-112



±56' TYPICAL SECTION - STAGE 2A
B-58-112

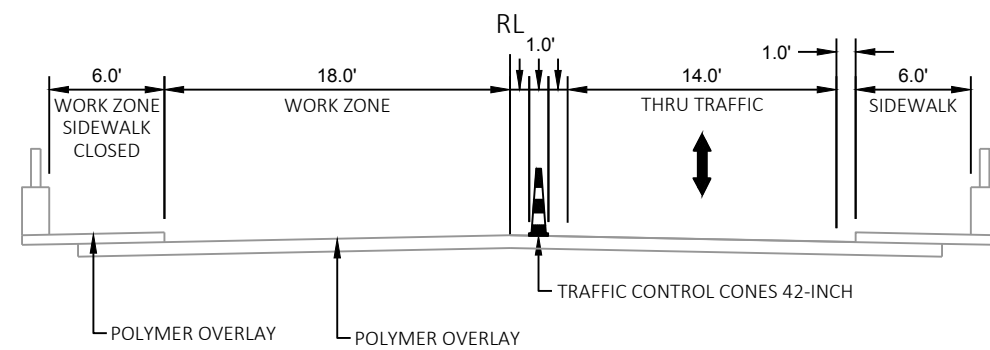


±56' TYPICAL SECTION - STAGE 2B
B-58-112



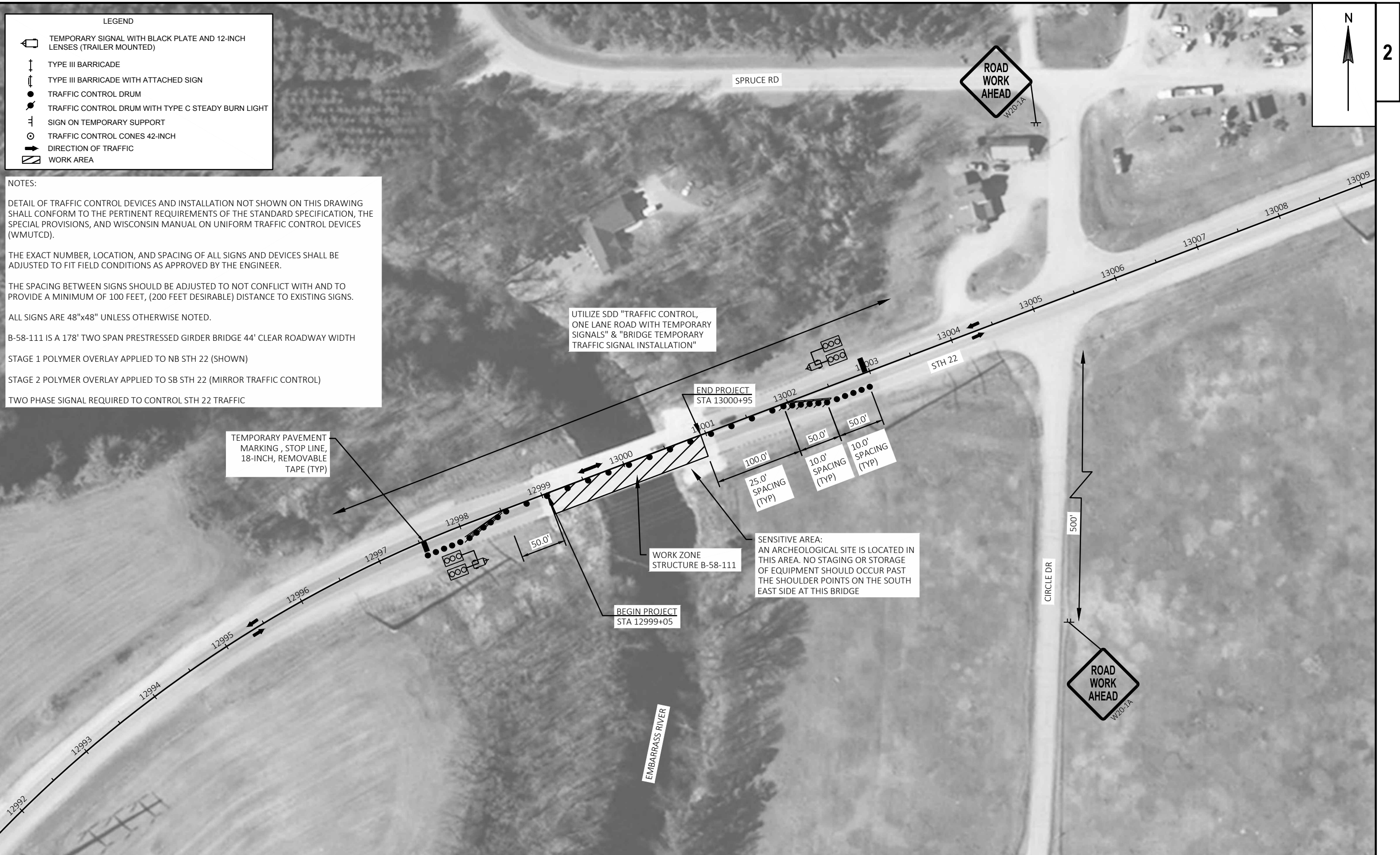
±48' TYPICAL SECTION - STAGE 1

B-58-124



±48' TYPICAL SECTION - STAGE 2

B-58-124



LEGEND

- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL CONES 42-INCH
- DIRECTION OF TRAFFIC
- WORK AREA

SIGNAL INCLUDES TEMPORARY
VEHICLE DETECTIONUTILIZE SDD "TRAFFIC CONTROL,
ONE LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"END PROJECT
STA 14001+69.95WORK ZONE
STRUCTURE B-58-112BEGIN PROJECT
STA 13998+30.05

WOLF RIVER

RIVER BEND RD

UTILIZE SDD "TRAFFIC CONTROL, ONE
LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"SIGNAL INCLUDES TEMPORARY
VEHICLE DETECTIONTEMPORARY PAVEMENT MARKING,
STOP LINE, 18-INCH, REMOVABLE TAPE (TYP)UTILIZE SDD "TRAFFIC CONTROL, ONE
LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"SIGNAL INCLUDES TEMPORARY
VEHICLE DETECTIONPLACE SIGNS AT
THE INTERSECTION
OF STH 29 EXIT
RAMPS AND STH 22

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-58-112 IS A 327.9' THREE SPAN 54" PRE-STRESSED GIRDER BRIDGE 44' CLEAR ROADWAY WIDTH

STAGE 1 POLYMER OVERLAY APPLIED TO NB STH 22.

FOUR PHASE TRAFFIC SIGNAL REQUIRED TO CONTROL STH 22 AND RIVER BEND RD TRAFFIC. INCLUDE TEMPORARY VEHICLE DETECTION ON ALL TEMPORARY SIGNALS.

PROJECT NO: 1009-47-62

HWY: VAR HWY

COUNTY: SHAWANO

TRAFFIC CONTROL - STH 22 (B-58-112) - STAGE 1

SHEET

E

LEGEND

- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL CONES 42-INCH
- DIRECTION OF TRAFFIC
- WORK AREA



SIGNAL INCLUDES TEMPORARY
VEHICLE DETECTION

UTILIZE SDD "TRAFFIC CONTROL,
ONE LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"

END PROJECT
STA 14001+69.95

WORK ZONE
STRUCTURE B-58-112

BEGIN PROJECT
STA 13998+30.05

WOLF RIVER

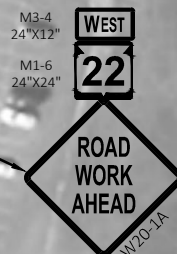
TEMPORARY PAVEMENT MARKING,
STOP LINE, 18-INCH, REMOVABLE TAPE (TYP)

UTILIZE SDD "TRAFFIC CONTROL, ONE
LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"

UTILIZE SDD "TRAFFIC CONTROL, ONE
LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"

SIGNAL INCLUDES TEMPORARY
VEHICLE DETECTION

PLACE SIGNS AT
THE INTERSECTION
OF STH 29 EXIT
RAMPS AND STH 22



NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-58-112 IS A 327.9' THREE SPAN 54" PRE-STRESSED GIRDER BRIDGE 44' CLEAR ROADWAY WIDTH.

STAGE 2A POLYMER OVERLAY APPLIED TO SB STH 22.

FOUR PHASE TRAFFIC SIGNAL REQUIRED TO CONTROL STH 22 AND RIVER BEND RD TRAFFIC. INCLUDE TEMPORARY VEHICLE DETECTION ON ALL TEMPORARY SIGNALS.

PROJECT NO: 1009-47-62

HWY: VAR HWY

COUNTY: SHAWANO










TRAFFIC CONTROL - STH 22 (B-58-112) - STAGE 2A

SHEET

E



LEGEND

-  TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL CONES 42-INCH
-  DIRECTION OF TRAFFIC
-  WORK AREA

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-58-116 IS A 256' 3 SPAN 45" PRE-STRESSED GIRDER BRIDGE 44' CLEAR ROADWAY WIDTH.

STAGE 1 POLYMER OVERLAY APPLIED TO NB STH 22 (SHOWN)

STAGE 2 POLYMER OVERLAY APPLIED TO SB STH 22 (MIRROR TRAFFIC CONTROL). ADJUST BUFFER ON WEST SIDE TO AVOID CONFLICT WITH DRIVEWAY.

TWO PHASE SIGNALS REQUIRED TO CONTROL STH 22 TRAFFIC

UTILIZE SDD "TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS" & "BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION"

END PROJECT
STA 15001+34

WORK ZONE
STRUCTURE B-58-116

TEMPORARY PAVEMENT MARKING, STOP
LINE, 18-INCH, REMOVABLE TAPE (TYP)

STOP
R1-1
30"X30"

PROJECT NO: 1009-47-62

HWY: VAR HWY










COUNTY: SHAWANO

TRAFFIC CONTROL - STH 22 (B-58-116)

SHEET

E

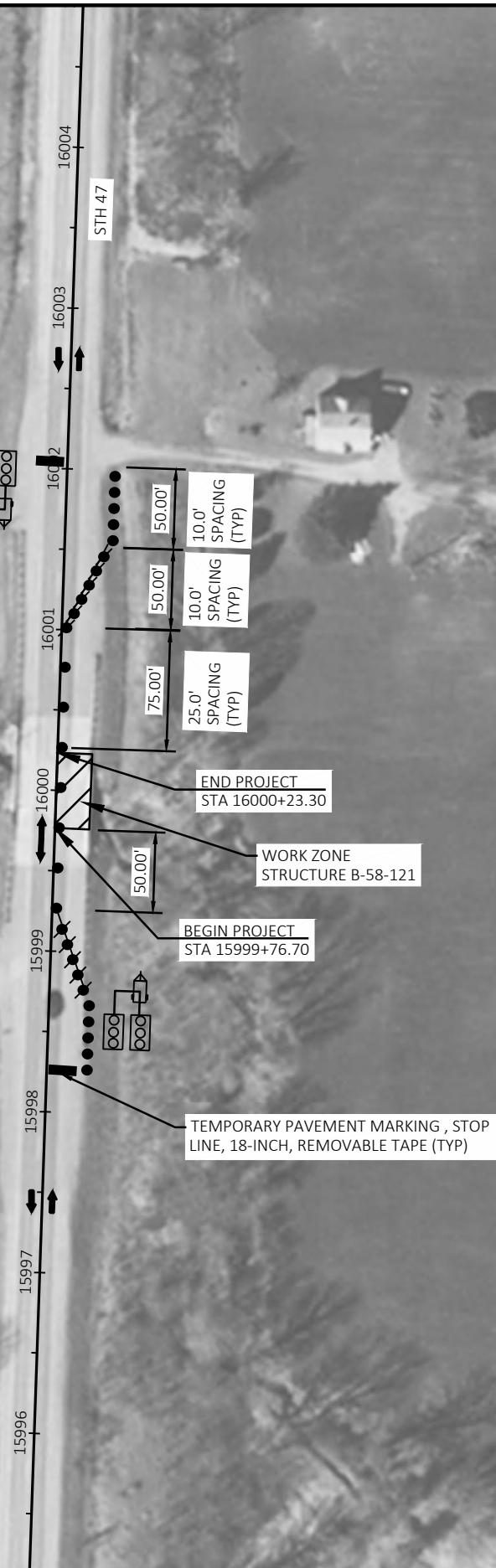
LEGEND

-  TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL CONES 42-INCH
-  DIRECTION OF TRAFFIC
-  WORK AREA



UTILIZE SDD "TRAFFIC CONTROL,
ONE LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"

STOP
R1-1
30"X30"



NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-58-121 IS A 34.6' SINGLE SPAN CONCRETE FLAT SLAB 44' CLEAR ROADWAY WIDTH

STAGE 1 POLYMER OVERLAY APPLIED TO NB STH 47 (SHOWN)

STAGE 2 POLYMER OVERLAY APPLIED TO SB STH 47 (MIRROR TRAFFIC CONTROL). ADJUST BUFFER ON SOUTH SIDE TO AVOID CONFLICT WITH DRIVEWAY.

TWO PHASE SIGNALS REQUIRED TO CONTROL STH 47 TRAFFIC.

PROJECT NO: 1009-47-62

HWY: VAR HWY

COUNTY: SHAWANO

TRAFFIC CONTROL - STH 47 (B-58-121)

SHEET

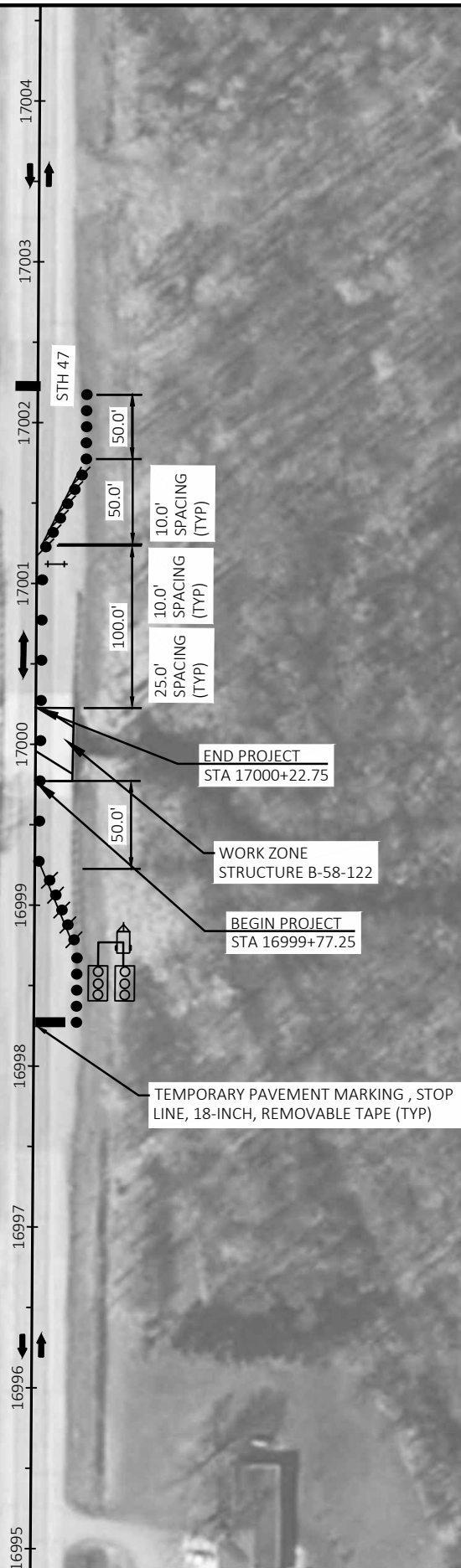
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LEGEND

- TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL CONES 42-INCH
- DIRECTION OF TRAFFIC
- WORK AREA



MINK CREEK

UTILIZE SDD "TRAFFIC CONTROL,
ONE LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-58-122 IS A 33.5' SINGLE SPAN CONCRETE FLAT SLAB 44' CLEAR ROADWAY WIDTH

STAGE 1 POLYMER OVERLAY APPLIED TO NB STH 47 (SHOWN)

STAGE 2 POLYMER OVERLAY APPLIED TO SB STH 47 (MIRROR TRAFFIC CONTROL)

TWO PHASE SIGNALS ARE REQUIRED TO CONTROL STH 47 TRAFFIC

PROJECT NO: 1009-47-62

HWY: VAR HWY










COUNTY: SHAWANO

TRAFFIC CONTROL - STH 47 (B-58-122)

SHEET

E

LEGEND

-  TEMPORARY SIGNAL WITH BLACK PLATE AND 12-INCH LENSES (TRAILER MOUNTED)
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL CONES 42-INCH
-  DIRECTION OF TRAFFIC
-  WORK AREA



SLAB CITY CREEK

END PROJECT
STA 18000+22.80WORK ZONE
STRUCTURE B-58-123BEGIN PROJECT
STA 17999+77.20TEMPORARY PAVEMENT MARKING , STOP
LINE, 18-INCH, REMOVABLE TAPE (TYP)UTILIZE SDD "TRAFFIC CONTROL,
ONE LANE ROAD WITH TEMPORARY
SIGNALS" & "BRIDGE TEMPORARY
TRAFFIC SIGNAL INSTALLATION"

NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-58-123 IS A 33.6' SINGLE SPAN CONCRETE FLAT SLAB BRIDGE 44' CLEAR ROADWAY WIDTH

STAGE 1 POLYMER OVERLAY APPLIED TO NB STH 47 (SHOWN)

STAGE 2 POLYMER OVERLAY APPLIED TO SB STH 47 (MIRROR TRAFFIC CONTROL)

TWO PHASE SIGNALS ARE REQUIRED TO CONTROL STH 47 TRAFFIC

PROJECT NO: 1009-47-62

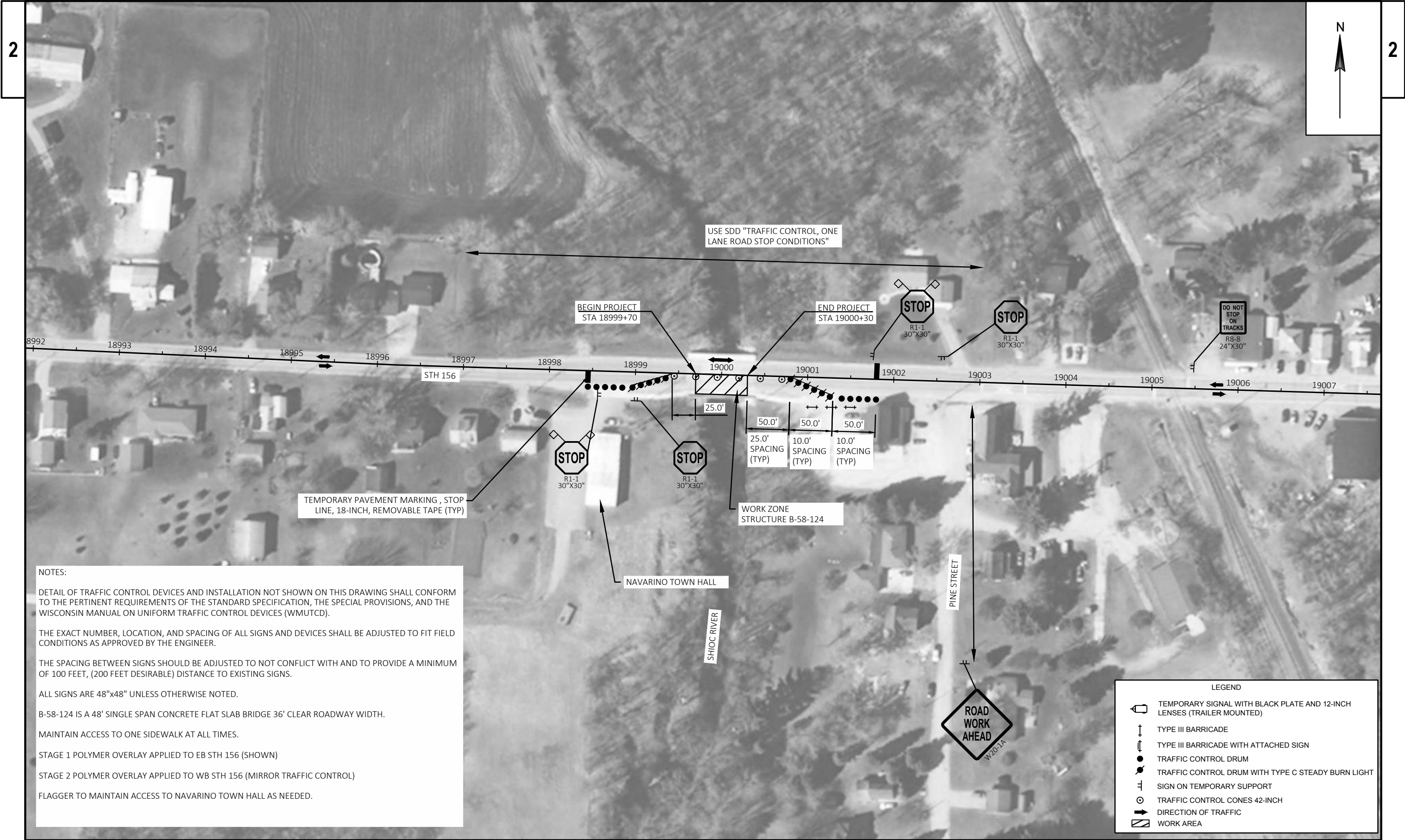
HWY: VAR HWY

COUNTY: SHAWANO

TRAFFIC CONTROL - STH 47 (B-58-123)

SHEET

E



NOTES:

DETAIL OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATION, THE SPECIAL PROVISIONS, AND THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET, (200 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

B-58-124 IS A 48' SINGLE SPAN CONCRETE FLAT SLAB BRIDGE 36' CLEAR ROADWAY WIDTH.

MAINTAIN ACCESS TO ONE SIDEWALK AT ALL TIMES.

STAGE 1 POLYMER OVERLAY APPLIED TO EB STH 156 (SHOWN)

STAGE 2 POLYMER OVERLAY APPLIED TO WB STH 156 (MIRROR TRAFFIC CONTROL)

FLAGGER TO MAINTAIN ACCESS TO NAVARINO TOWN HALL AS NEEDED.

Estimate Of Quantities By Plan Sets

1009-47-62					
Line	Item	Item Description	Unit	Total	Qty
0002	509.5100.S	Polymer Overlay	SY	4,931.000	4,931.000
0006	618.0100	Maintenance And Repair of Haul Roads (project) 02. 1009-47-62	EACH	1.000	1.000
0008	619.1000	Mobilization	EACH	0.400	0.400
0010	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0012	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0014	628.7010	Inlet Protection Type B	EACH	6.000	6.000
0016	628.7015	Inlet Protection Type C	EACH	8.000	8.000
0024	642.5001	Field Office Type B 03. STH 22, STH 47 and STH 156	EACH	1.000	1.000
0026	643.0300	Traffic Control Drums	DAY	1,644.000	1,644.000
0028	643.0410	Traffic Control Barricades Type II	DAY	16.000	16.000
0030	643.0420	Traffic Control Barricades Type III	DAY	48.000	48.000
0034	643.0715	Traffic Control Warning Lights Type C	DAY	570.000	570.000
0036	643.0900	Traffic Control Signs	DAY	1,162.000	1,162.000
0038	643.1070	Traffic Control Cones 42-Inch	DAY	36.000	36.000
0040	643.5000	Traffic Control	EACH	0.400	0.400
0042	644.1420.S	Temporary Pedestrian Surface Plywood	SF	32.000	32.000
0044	644.1601.S	Temporary Curb Ramp	EACH	2.000	2.000
0046	644.1810.S	Temporary Pedestrian Barricade	LF	490.000	490.000
0048	646.1020	Marking Line Epoxy 4-Inch	LF	5,650.000	5,650.000
0052	646.7120	Marking Diagonal Epoxy 12-Inch	LF	35.000	35.000
0056	646.9000	Marking Removal Line 4-Inch	LF	3,585.000	3,585.000
0060	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	19,600.000	19,600.000
0062	649.0850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	224.000	224.000
0076	661.0100	Temporary Traffic Signals for Bridges (structure) 07. B-58-0111	LS	1.000	1.000
0078	661.0100	Temporary Traffic Signals for Bridges (structure) 08. B-58-0112	LS	1.000	1.000
0080	661.0100	Temporary Traffic Signals for Bridges (structure) 09. B-58-0116	LS	1.000	1.000
0082	661.0100	Temporary Traffic Signals for Bridges (structure) 10. B-58-0121	LS	1.000	1.000
0084	661.0100	Temporary Traffic Signals for Bridges (structure) 11. B-58-0122	LS	1.000	1.000
0086	661.0100	Temporary Traffic Signals for Bridges (structure) 12. B-58-0123	LS	1.000	1.000
0092	SPV.0105	Special 01. Temporary Vehicle Detection B-58-0112	LS	1.000	1.000

MOBILIZATION

STATION	619.1000 EACH
PROJECT	0.4
ITEM TOTAL	0.4

FIELD OFFICE TYPE B

STATION	642.5001 EACH
PROJECT	1
ITEM TOTAL	1

TRAFFIC CONTROL

	643.0300	643.0410	644.0420	643.0715	643.0900	643.1070	644.1420.S	644.1601.S	644.1810.S	646.9000	649.0150	649.0850	661.0100	SPV.0105.01
		BARRICADES	BARRICADES	WARNING			TEMPORARY	TEMPORARY	TEMPORARY	MARKING	TEMPORARY	TEMPORARY	TEMPORARY	
		TYPE II	TYPE III	LIGHTS		CONES	PEDESTRIAN	CURB	PEDESTRIAN	REMOVAL	MARKING	MARKING	TRAFFIC	TEMPORARY
				TYPE C	SIGNS	42-INCH	SURFACE	RAMP	BARRICADE	LINE	REMOVABLE TAPE	REMOVABLE TAPE	SIGNAL FOR	VEHICLE
STATION	DRUMS	TYPE II	TYPE III	TYPE C	DAY	EACH	PLYWOOD	EACH	LF	4-INCH	4-INCH	18-INCH	(STRUCTURE)	DETECTION
	DAY	DAY	DAY	DAY			SF			LF	LF	LF	LS	LS
STH 22 B-58-111	288	-	8	96	176	-	-	-	-	1080	2800	28	1	-
STH 22 B-58-112	402	16	8	96	296	-	32	2	490	360	2800	56	1	1
STH 22 B-58-116	312	-	8	96	168	-	-	-	-	375	2800	28	1	-
STH 47 B-58-121	168	-	6	72	126	-	-	-	-	100	2800	28	1	-
STH 47 B-58-122	174	-	6	72	120	-	-	-	-	500	2800	28	1	-
STH 47 B-58-123	174	-	6	72	120	-	-	-	-	500	2800	28	1	-
STH 156 B-58-124	126	-	6	66	156	36	-	-	-	670	2800	28	-	-
ITEM TOTAL	1644	16	48	570	1162	36	32	2	490	3585	19600	224	6	1

EROSION CONTROL ITEMS

	628.1905	628.1910	628.7010	628.7015
	MOBILIZATIONS	MOBILIZATIONS	INLET	INLET
	EROSION	EMERGENCY	PROTECTION	PROTECTION
	CONTROL	EROSION	TYPE B	TYPE C
STATION	EACH	CONTROL	EACH	EACH
STH 22 B-58-111	1	1	2	-
STH 22 B-58-112	1	1	-	6
STH 22 B-58-116	1	1	4	-
STH 47 B-58-121	-	-	-	-
STH 47 B-58-122	-	-	-	-
STH 47 B-58-123	-	-	-	-
STH 156 B-58-124	1	1	-	2
ITEM TOTAL	4	4	6	8

MARKING LINE

		646.1020		646.712	
		EPOXY		DIAGONAL	
		4-INCH		EPOXY	
				12-INCH	
STATION	LOCATION	(WHITE) LF	(SKIP WHITE) LF	(YELLOW) LF	(DOUBLE YELLOW) LF
STH 22 B-58-111	LT & RT	390	-	-	1080
STH 22 B-58-112	LT & RT	680	15	-	360
STH 22 B-58-116	LT & RT	560	-	225	150
STH 47 B-58-121	LT & RT	100	-	-	100
STH 47 B-58-122	LT & RT	100	-	400	100
STH 47 B-58-123	LT & RT	100	-	400	100
STH 156 B-58-124	LT & RT	120	-	-	670
SUBTOTAL		2050	15	1025	450
ITEM TOTAL				2110	35

NOTE: ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR
ENGINEER ESTIMATE CATEGORY 0010 UNLESS OTHERWISE NOTED

PROJECT NO: 1009-47-62

HWY: VAR HWY

COUNTY: SHAWANO

MISCELLANEOUS QUANTITIES

SHEET

E

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
09G02-05C	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
15C02-07F	ADVANCED WIDTH RESTRICTION SIGNING
15C06-09	SIGNING & MARKING FOR TWO LANE BRIDGES
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15D30-04A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-04B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-04C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D32-05	TRAFFIC CONTROL, ONE LANE ROAD STOP CONDITION
15D33-05	TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



INLET PROTECTION, TYPE A

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



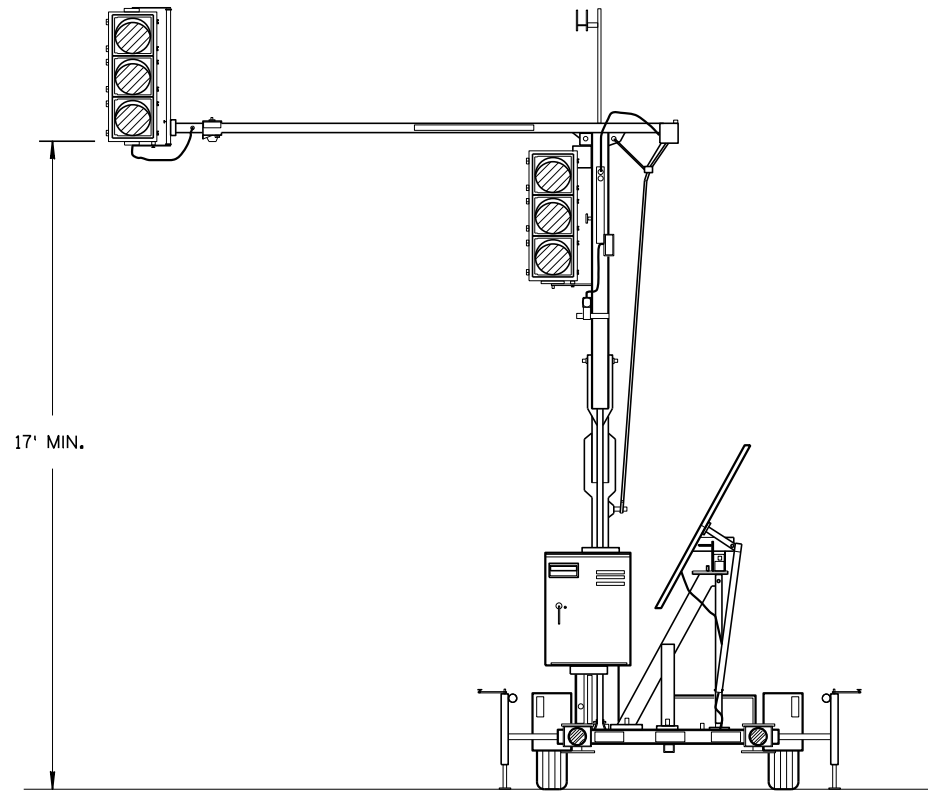
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Cannestra
DATE
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER

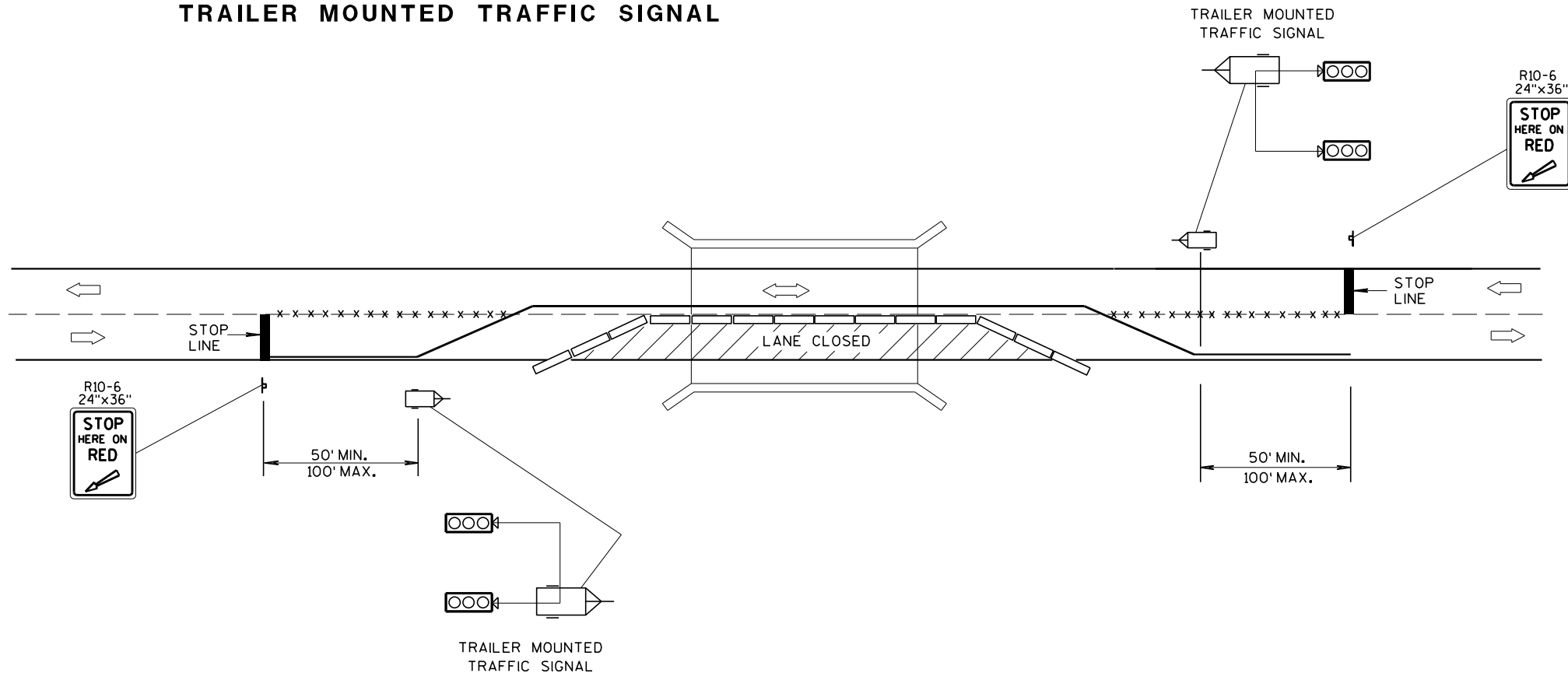


TRAILER MOUNTED TRAFFIC SIGNAL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15 D 33.



TYPICAL TRAILER MOUNTED TRAFFIC SIGNAL LOCATION

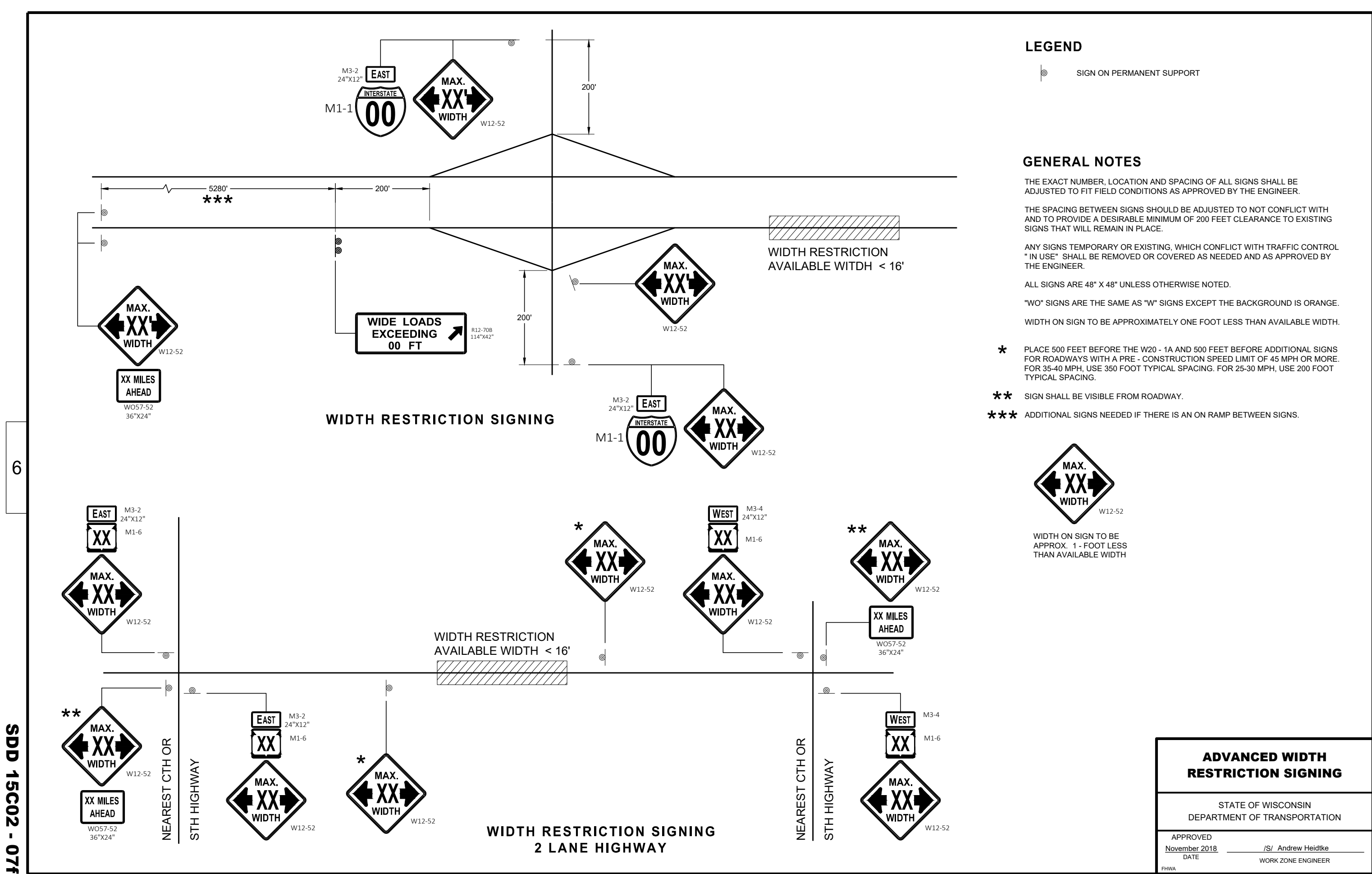
LEGEND

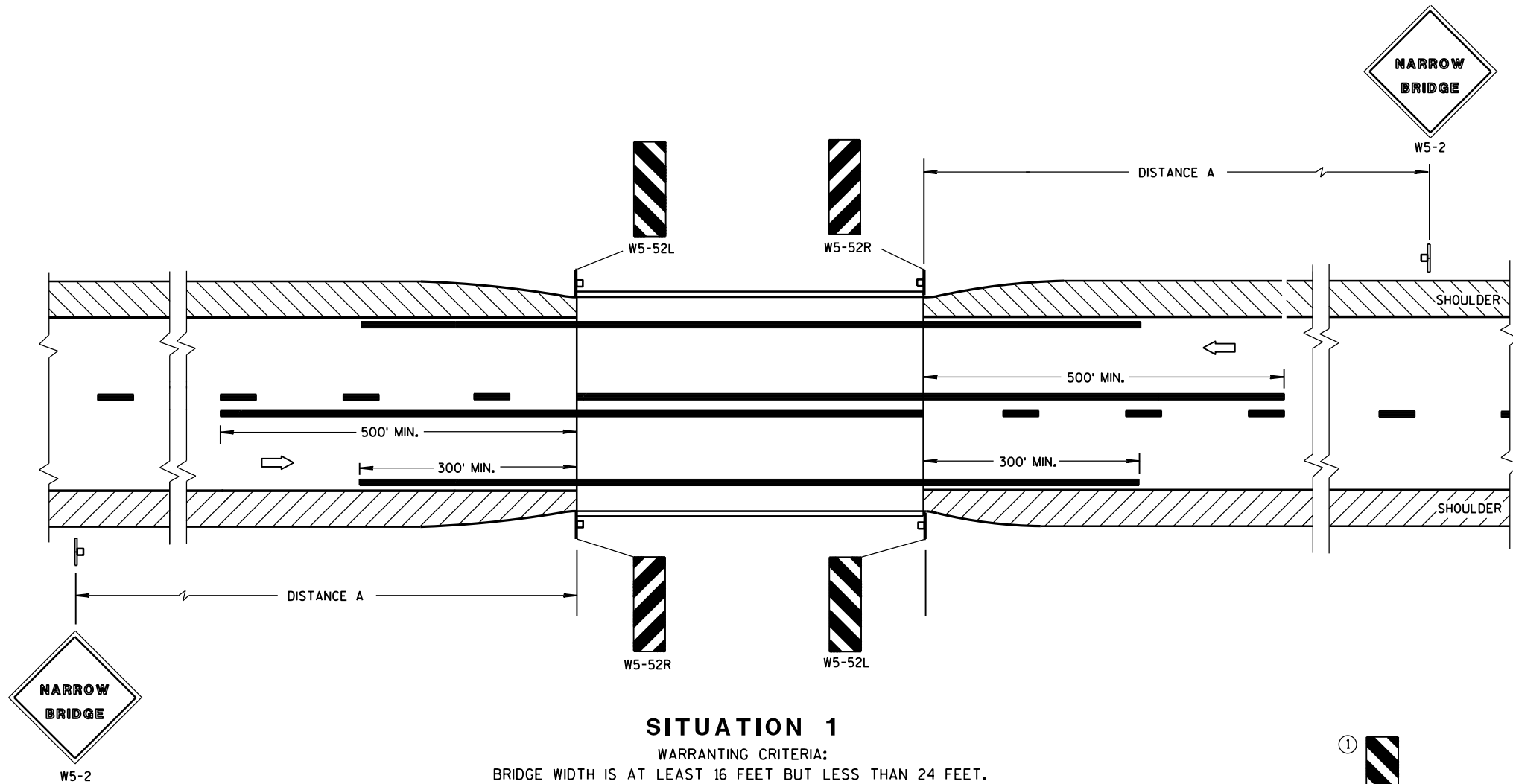
- POST MOUNTED SIGN
- REMOVING PAVEMENT MARKING
- TEMPORARY PRECAST CONCRETE BARRIER
- TRAILER MOUNTED TRAFFIC SIGNAL
- DIRECTION OF TRAFFIC FLOW

BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Ahmet Demirbilek
DATE STATE ELECTRICAL ENGINEER
FHWA





SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

GENERAL NOTES

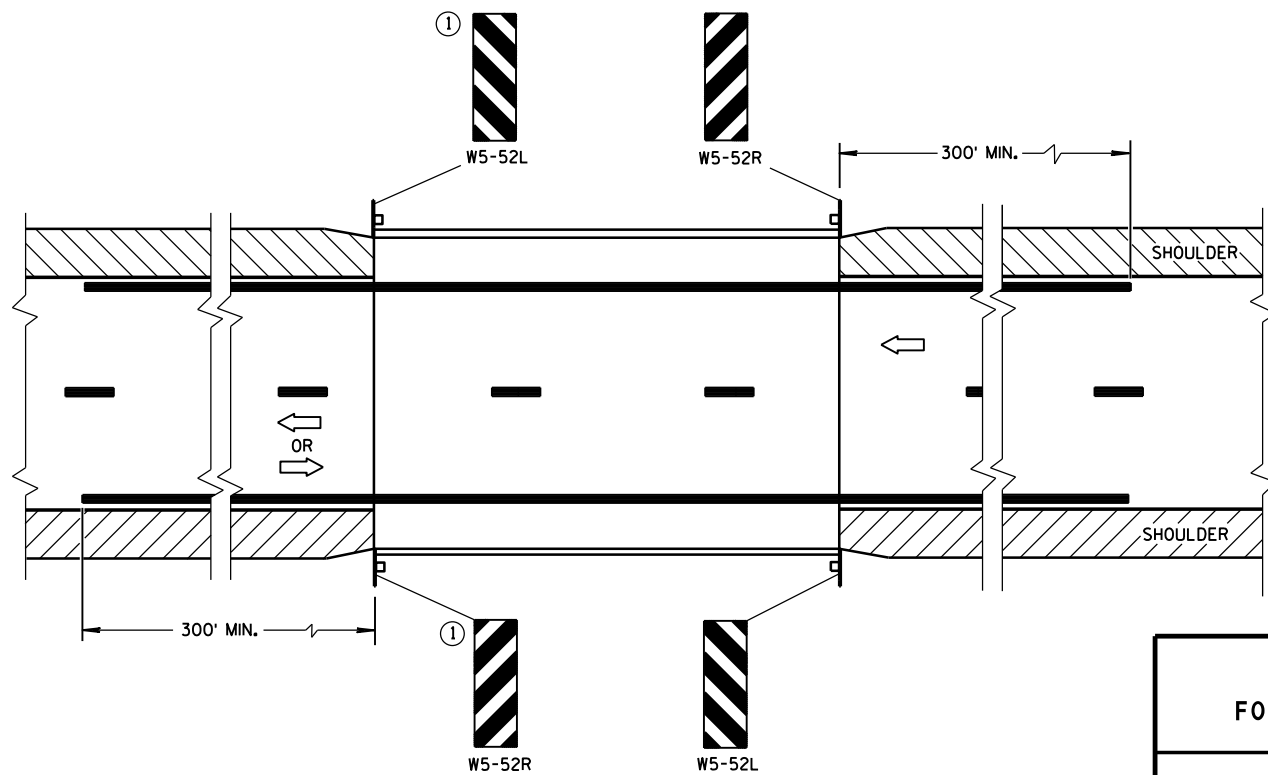
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

① OMIT ON ONE-WAY TRAVELLED WAYS.

➡ DIRECTION OF TRAFFIC



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

SIGNING & MARKING FOR TWO LANE BRIDGES

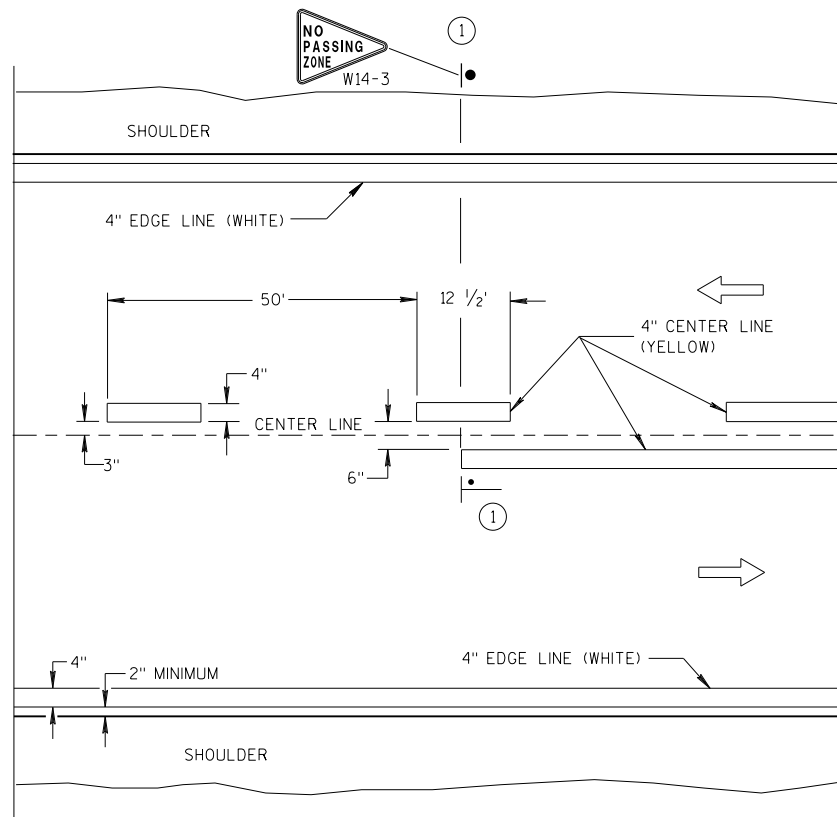
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

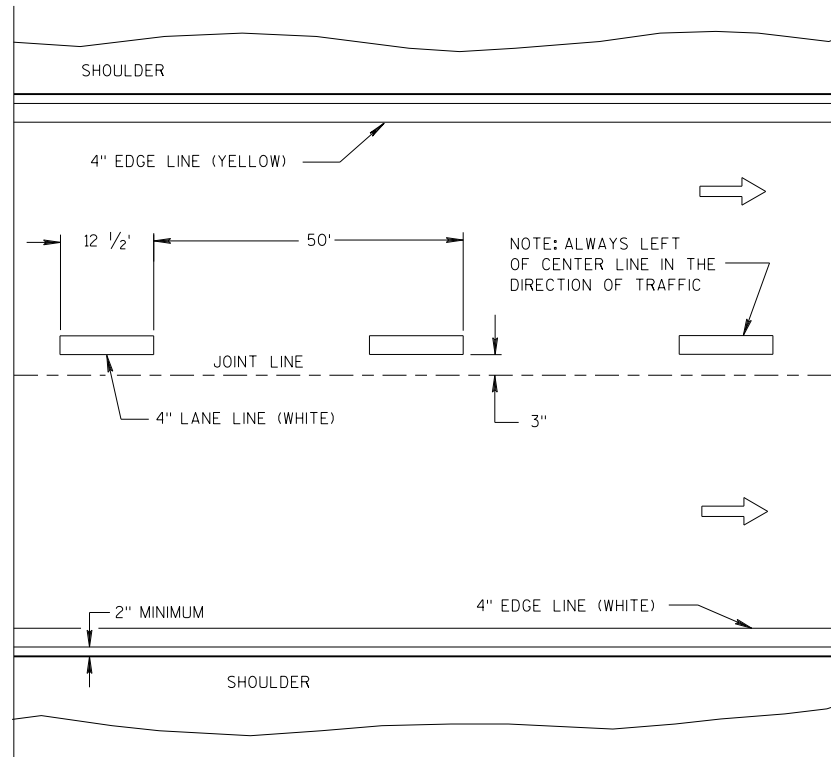
June 2017
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER

FHWA

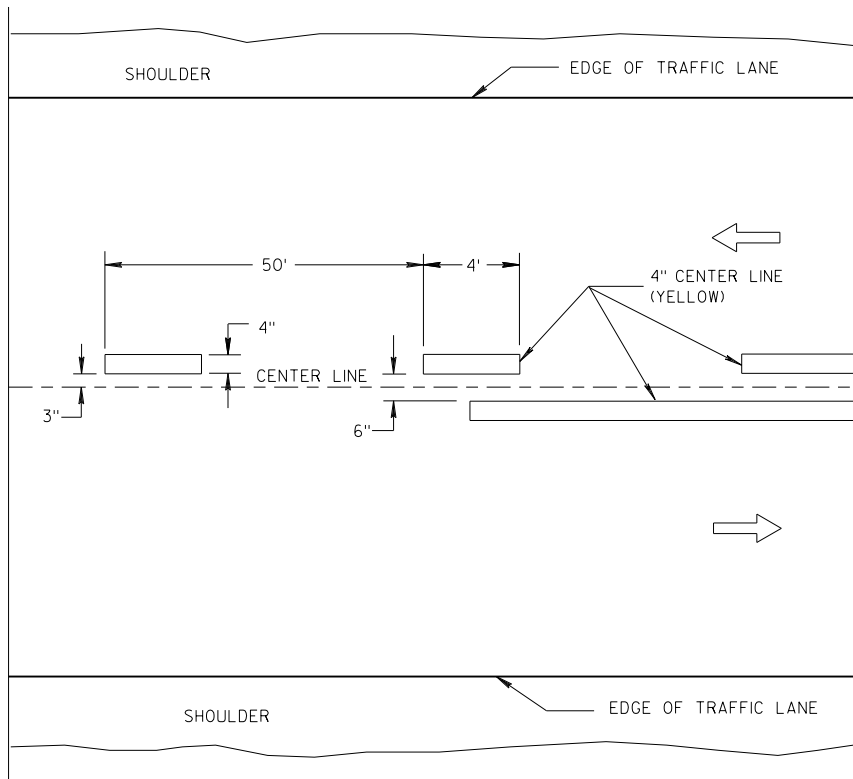


TWO WAY TRAFFIC

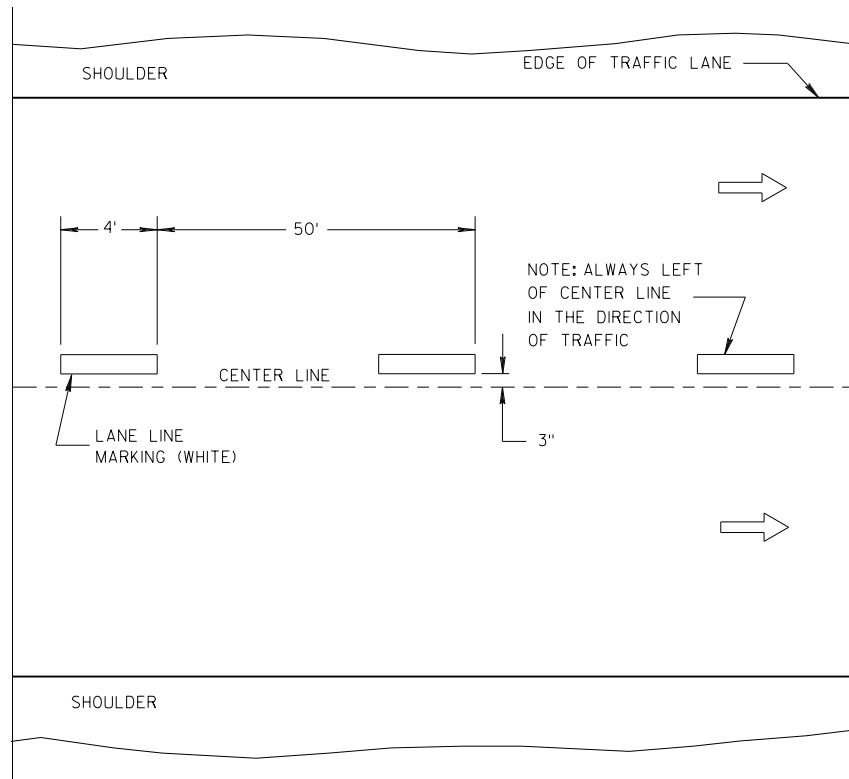


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

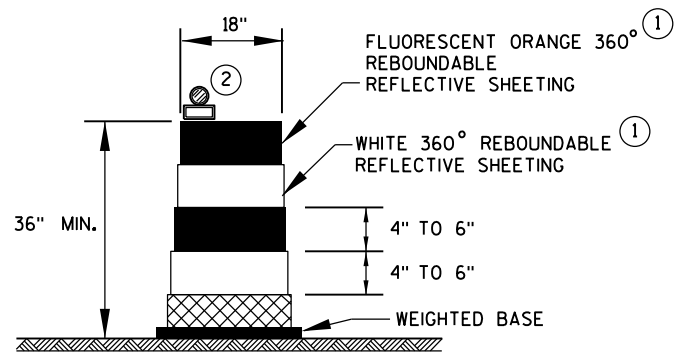
LEGEND

- "T" MARKING
- POST MOUNTED SIGN

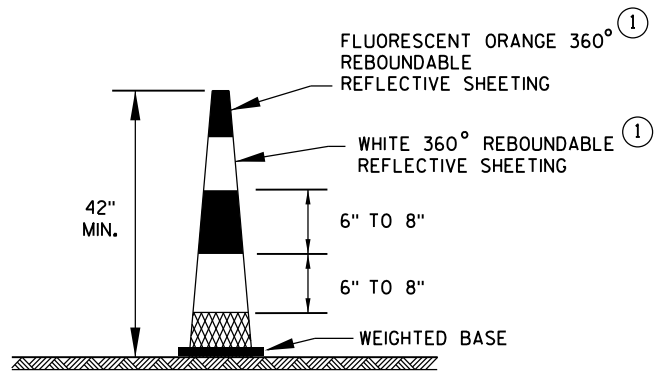
LONGITUDINAL MARKING
(MAINLINE)

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DEPARTMENT OF TRANSPORTATION

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7/2018 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA



DRUM

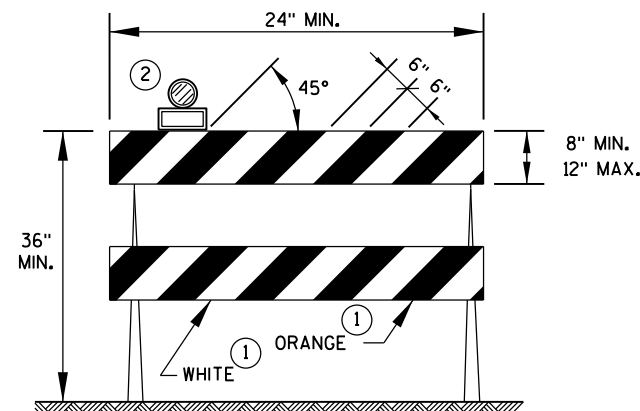


42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS

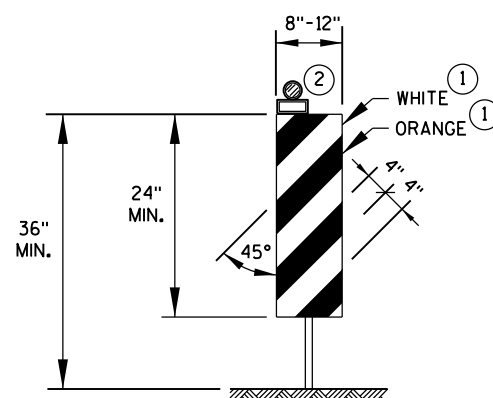
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



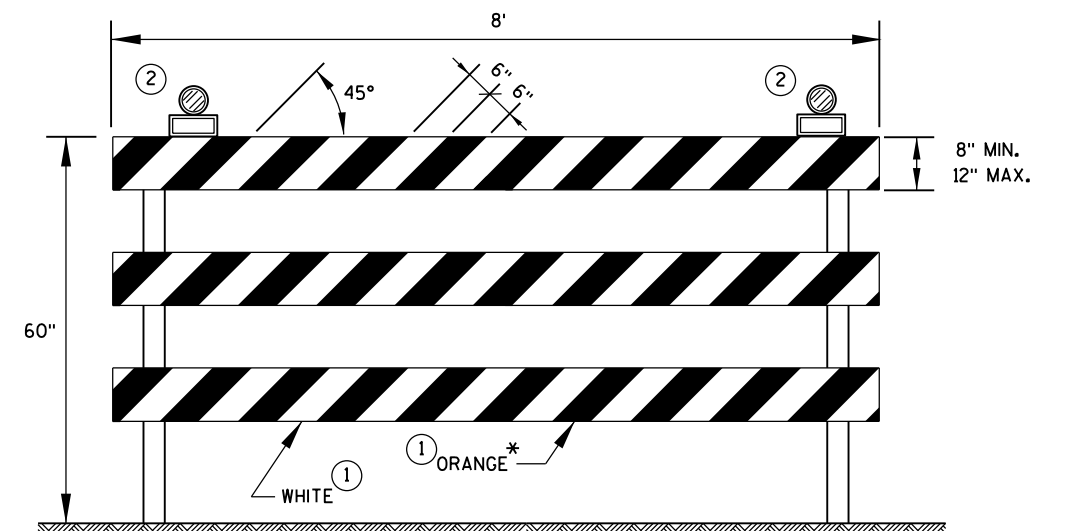
TYPE 2 BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE 3 BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

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DEPARTMENT OF TRANSPORTATION

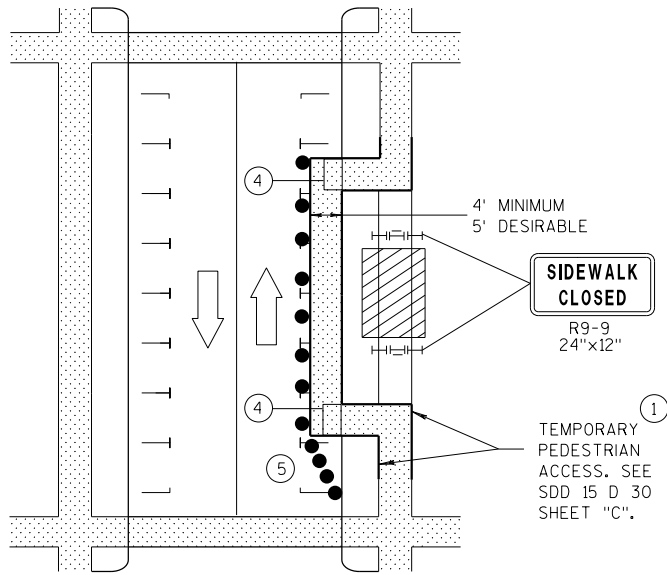
APPROVED

June 2017
DATE

FHWA

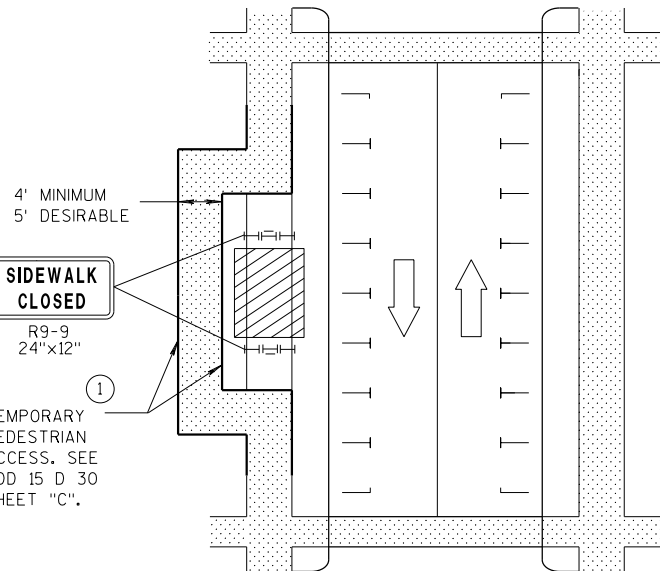
/S/ Andrew Heidtke
WORK ZONE ENGINEER

NOTE: MAY BE USED ON ROADWAY WITH POSTED SPEED OF LESS THAN 40 MPH.

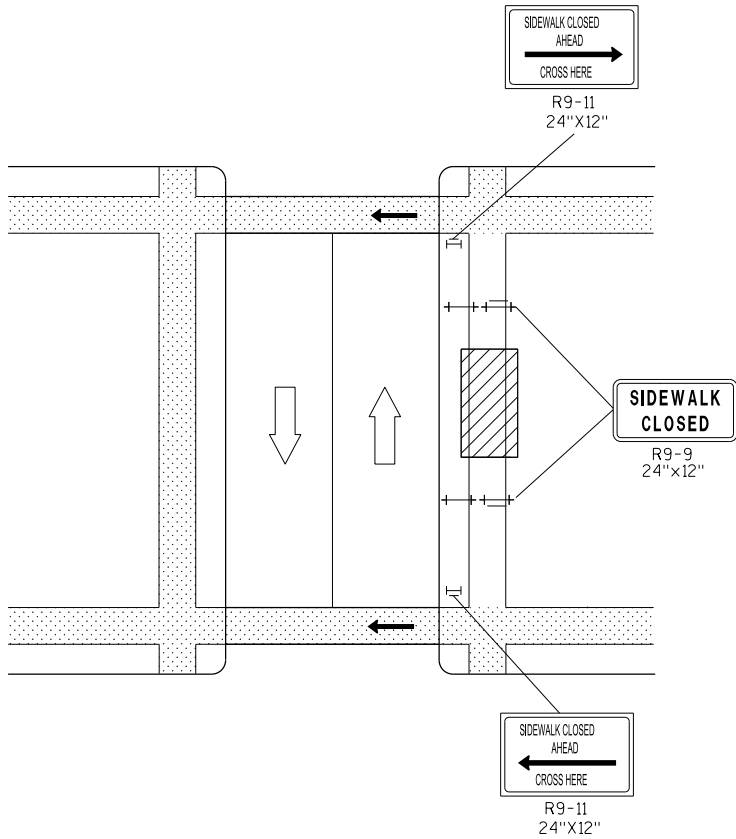


MID-BLOCK SIDEWALK CLOSURE
IN PARKING LANE

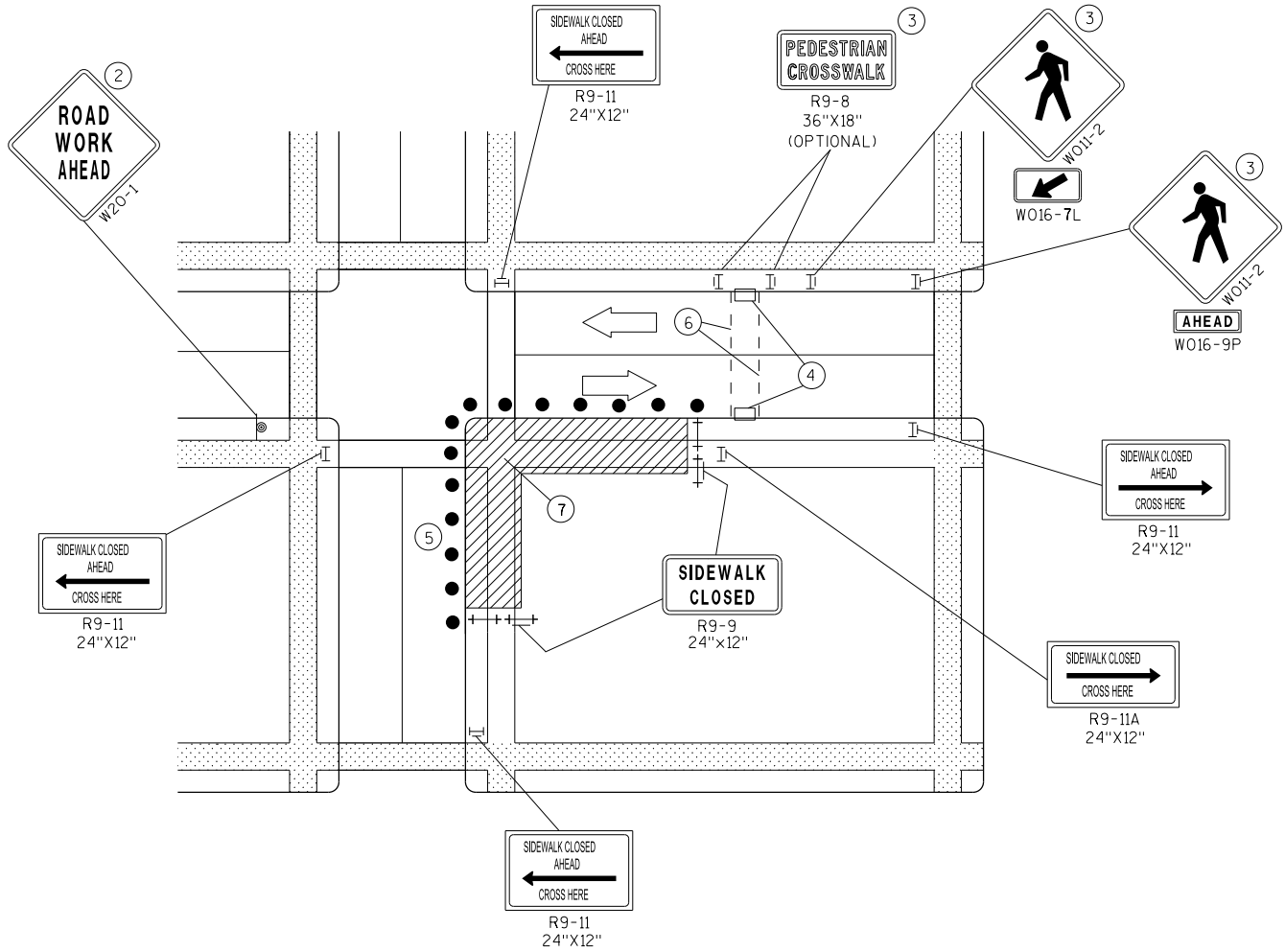
NOTE: LAYOUT SAME AS ABOVE.



SIDEWALK DIVERSION



MID-BLOCK SIDEWALK CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.
- "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND WO11-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.
- TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".
- DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

LEGEND

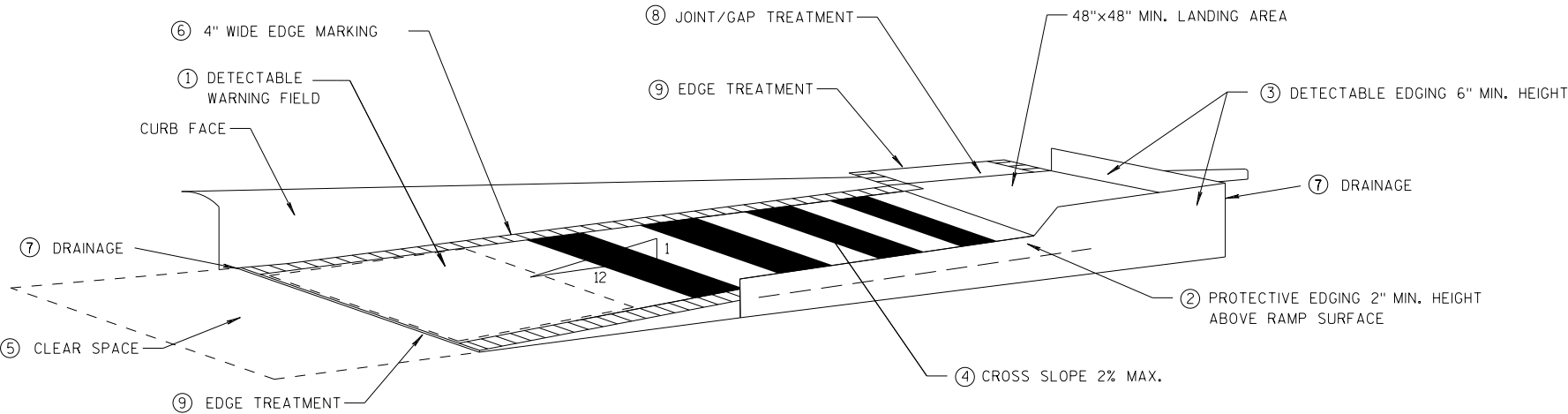
	SIGN ON PERMANENT SUPPORT		DIRECTION OF TRAFFIC
	UNDER PEDESTRIAN TRAFFIC		TRAFFIC CONTROL DRUM
	WORK AREA		
	PEDESTRIAN CHANNELIZATION DEVICE		
	TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		
	TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

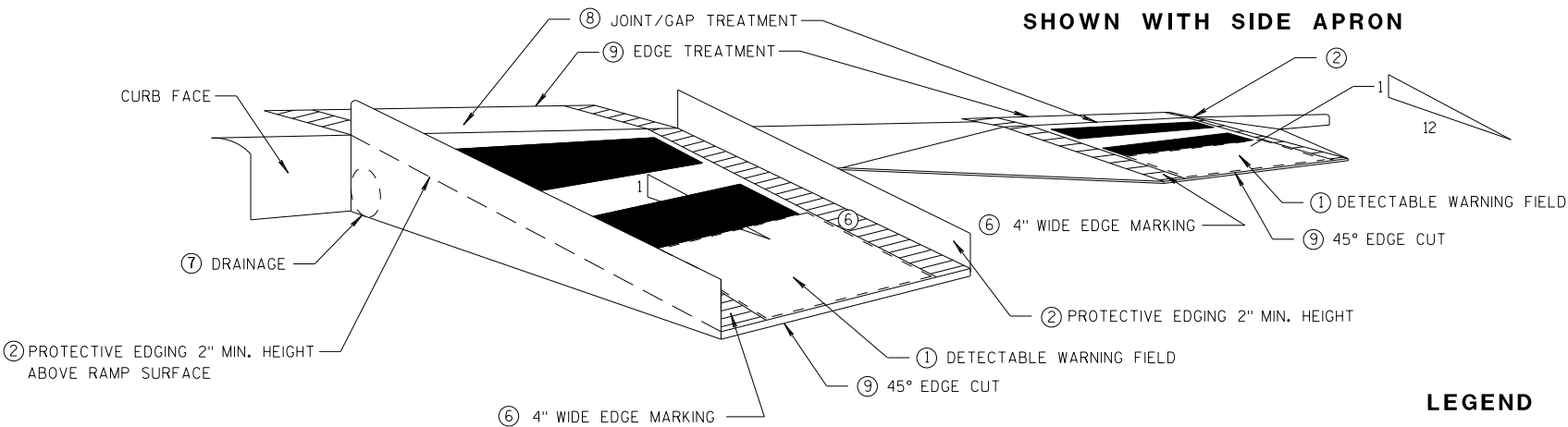
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.
- 1 CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 805 SHEET "E".
 - 2 PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
 - 3 DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
 - 4 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 - 5 CLEAR SPACE OF 48"x48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 - 6 THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
 - 7 DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
 - 8 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 - 9 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
 - 10 5' WIDE MIN. WITH PEDSETRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.

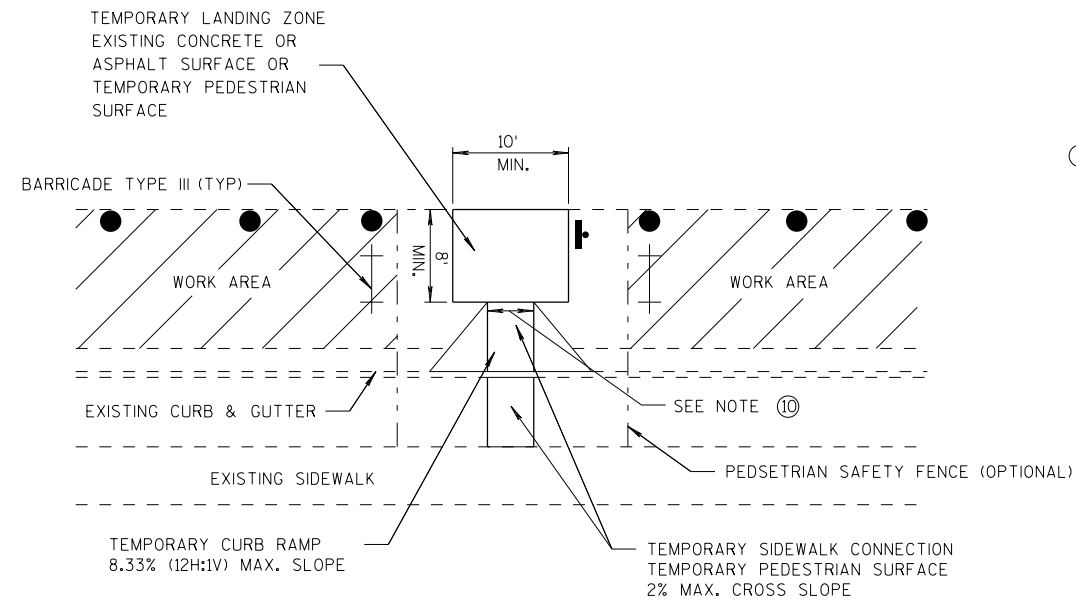


TEMPORARY CURB RAMP
PARALLEL TO CURB



SHOWN WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP
PERPENDICULAR TO CURB

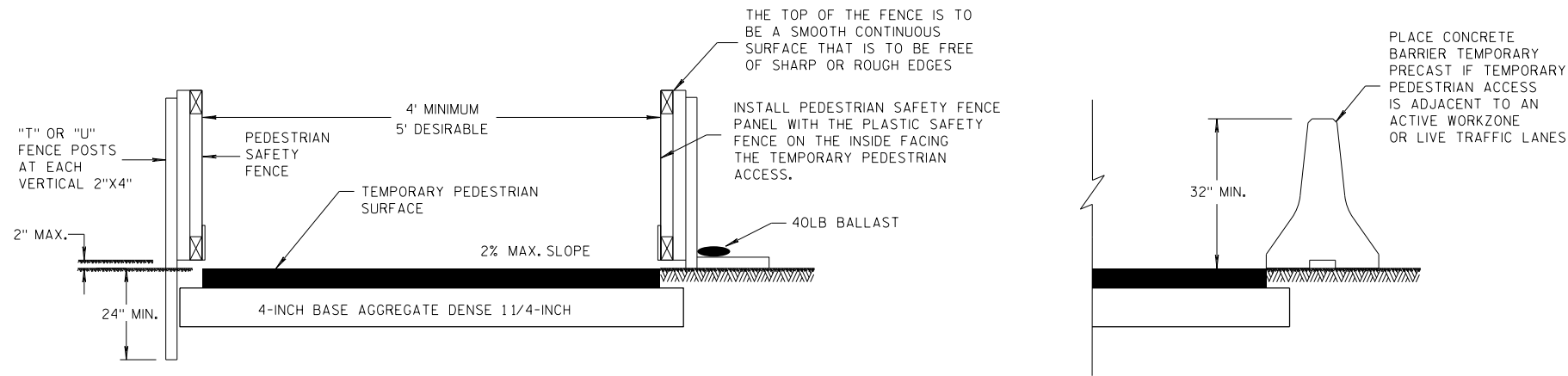


TEMPORARY BUS STOP PAD

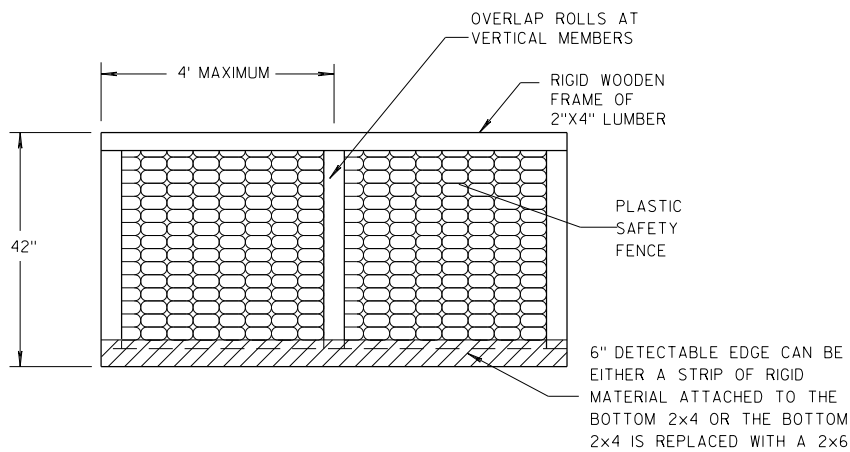
LEGEND

	WORK AREA
	TYPE III BARRICADE
	TRAFFIC CONTROL DRUM

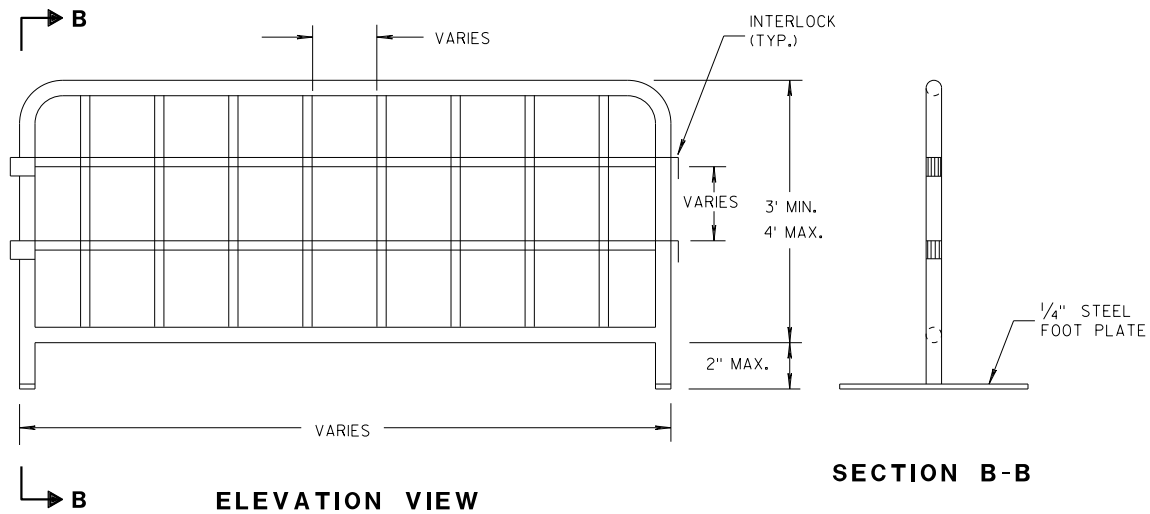
TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



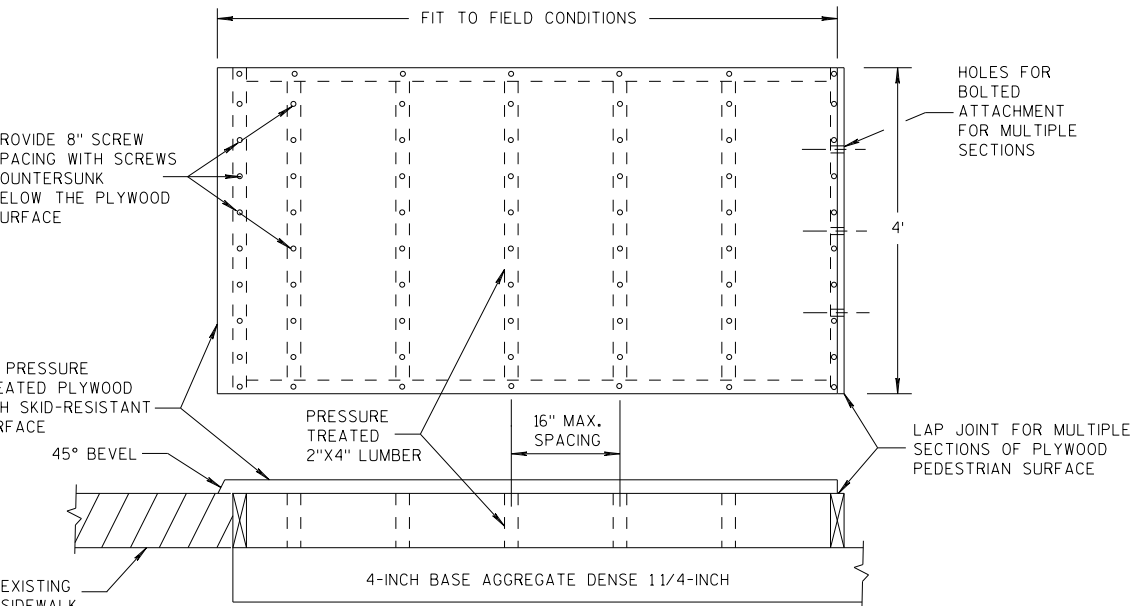
TEMPORARY PEDESTRIAN ACCESS



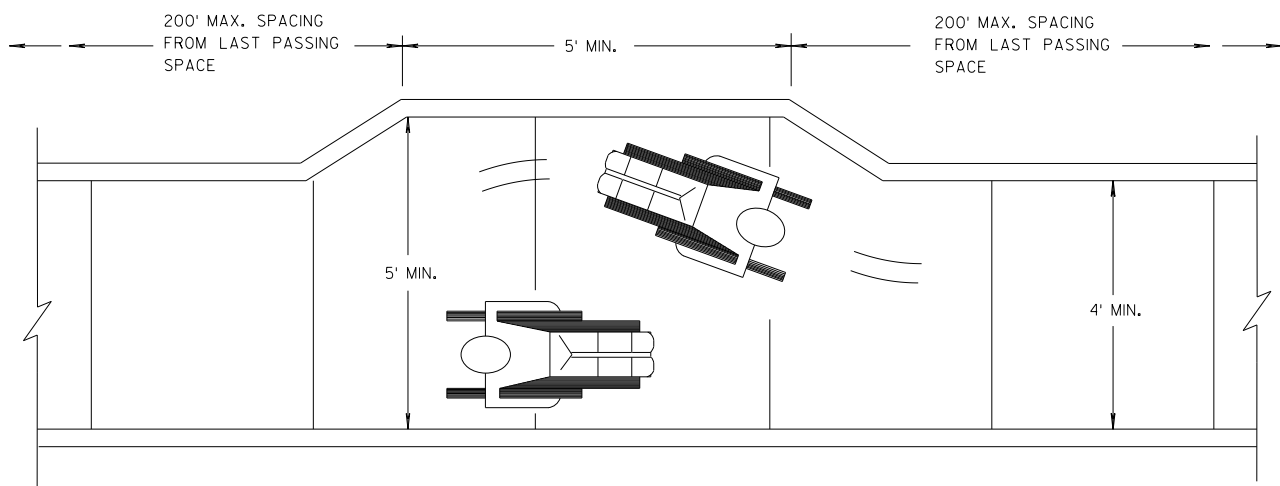
PEDESTRIAN SAFETY FENCE



TEMPORARY PEDESTRIAN STEEL BARRICADE

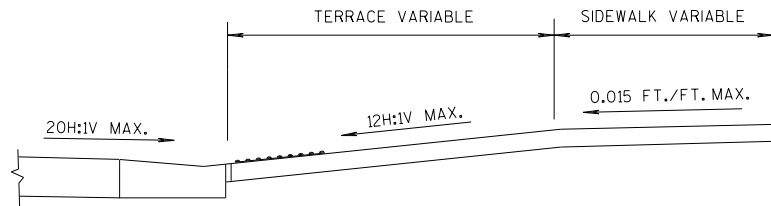


TEMPORARY PEDESTRIAN SURFACE PLYWOOD

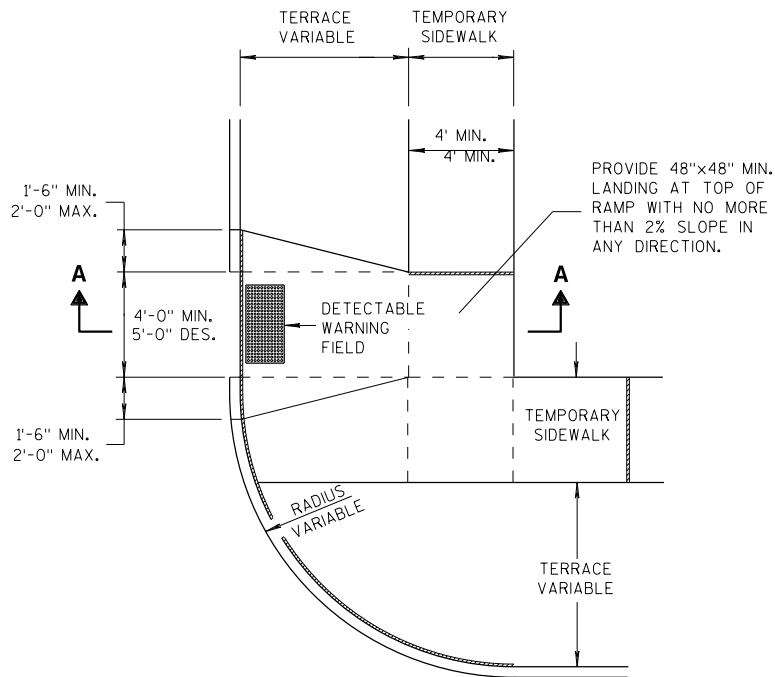


NARROW SIDEWALK PASSING DETAIL

- GENERAL NOTES**
- ① INTERCHANGEABLE WITH THE PEDESTRIAN SAFETY FENCE.












SECTION A-A

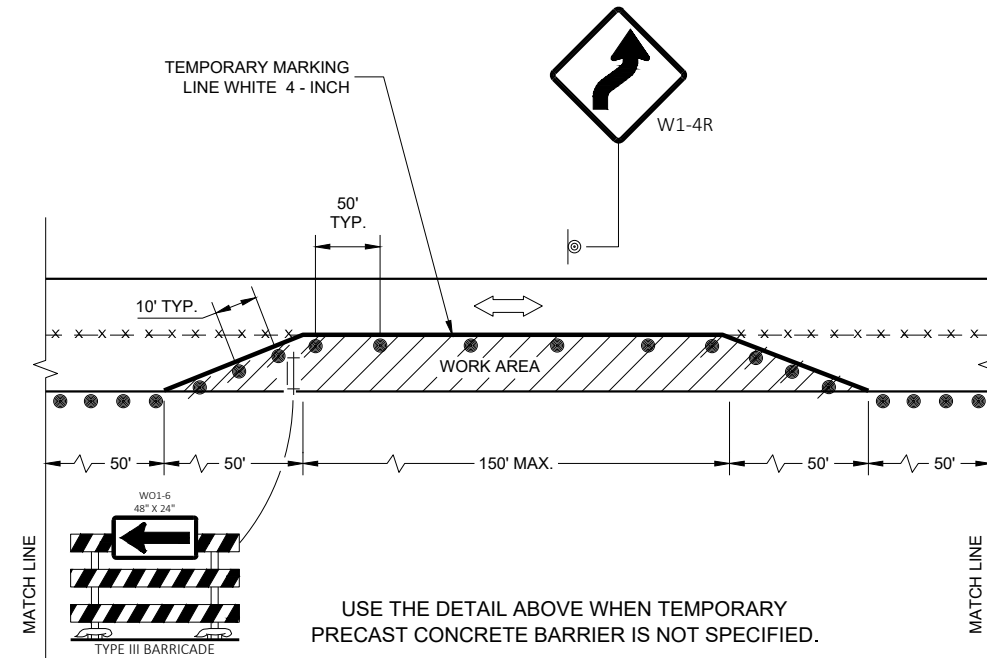


PLAN VIEW
TEMPORARY TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

- | | |
|---|--|
|  | TYPE III BARRICADE WITH ATTACHED SIGN |
|  | SIGN ON PERMANENT SUPPORT |
|  | TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT |
|  | TRAFFIC CONTROL DRUM |
|  | FLAGS, 16" X 16" MIN. (ORANGE) |
|  | REMOVING PAVEMENT MARKING |
|  | DIRECTION OF TRAFFIC |
|  | ASPHALTIC PAVEMENT WIDENING |
|  | CONCRETE BARRIER TEMPORARY PRECAST |



GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

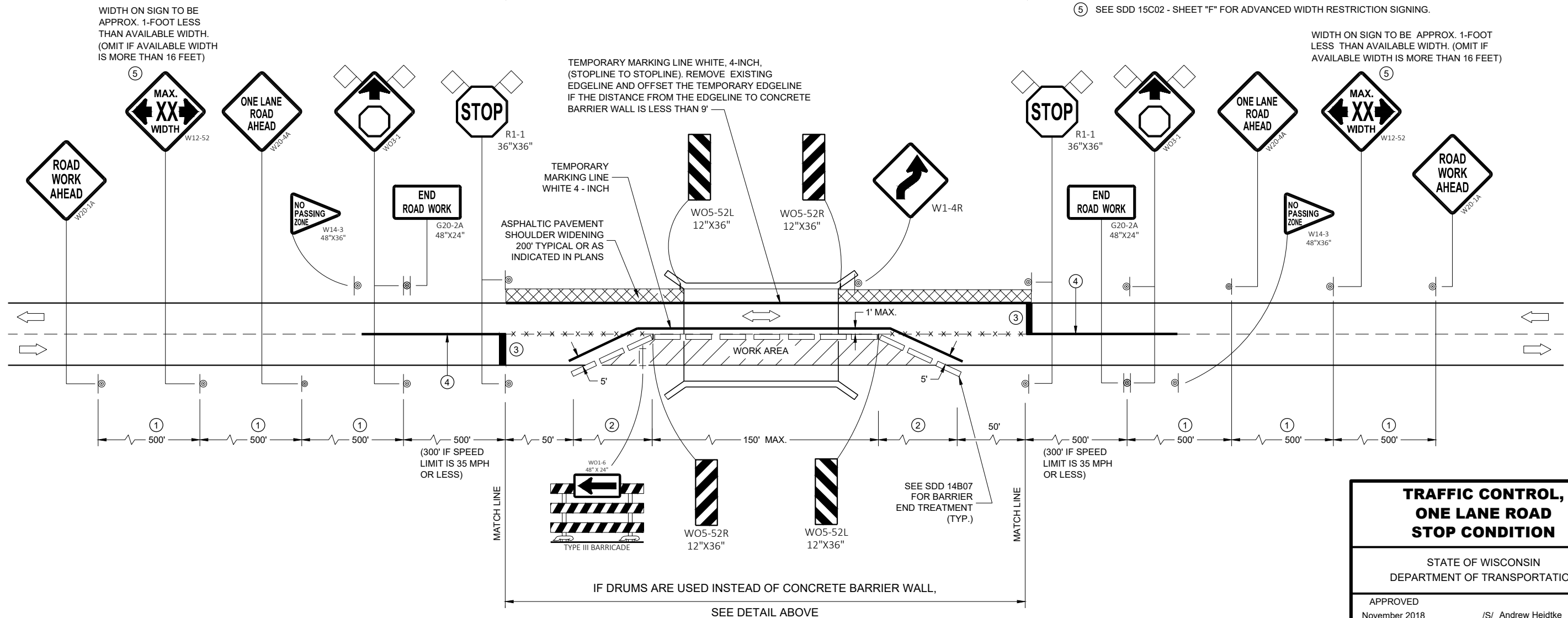
ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.

- ① 500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.
- ② DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.
- ③ TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18 - INCH.
- ④ 700 FOOT TEMPORARY MARKING LINE, DOUBLE YELLOW 4 - INCH . WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
- ⑤ SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.





TRAFFIC CONTROL, ONE LANE ROAD STOP CONDITION


STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION


APPROVED
November 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

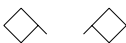
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
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
TYPE III BARRICADE WITH ATTACHED SIGN
- 


SIGN ON PERMANENT SUPPORT
- 


TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- 


TRAFFIC CONTROL DRUM
- 

FLAGS, 16" X 16" MIN. (ORANGE)
- 

REMOVING PAVEMENT MARKING
- 

DIRECTION OF TRAFFIC
- 

ASPHALTIC PAVEMENT WIDENING
- 

CONCRETE BARRIER TEMPORARY PRECAST
- 

TEMPORARY SIGNAL. SEE SDD 09G02 FOR EXACT PLACEMENT

WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)

- ① 500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.

② USE 300 FOOT SPACING IF THE PRE - CONSTRUCTION REGULATORY SPEED IS 35 MPH OR LESS.

③ DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.

④ TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18 - INCH.

⑤ 700 FOOT TEMPORARY MARKING DOUBLE YELLOW LINE 4 - INCH. WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.

⑥ SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..

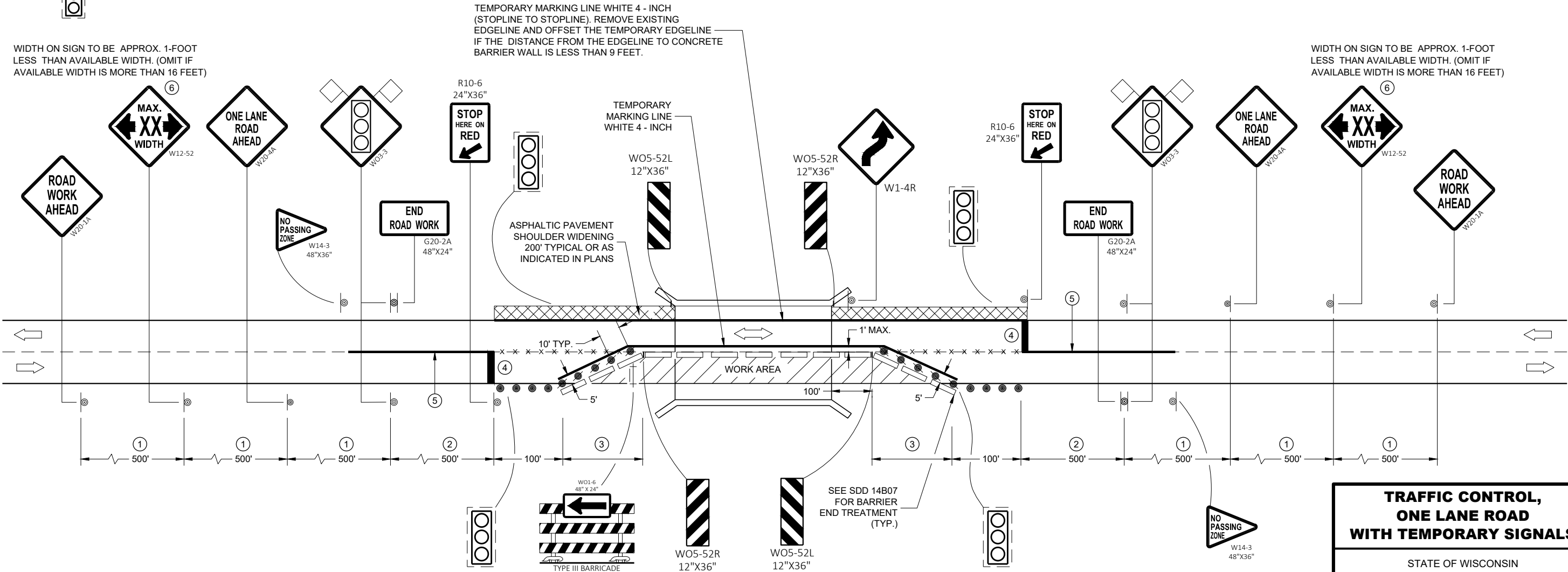
THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.



**TRAFFIC CONTROL,
ONE LANE ROAD
WITH TEMPORARY SIGNALS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

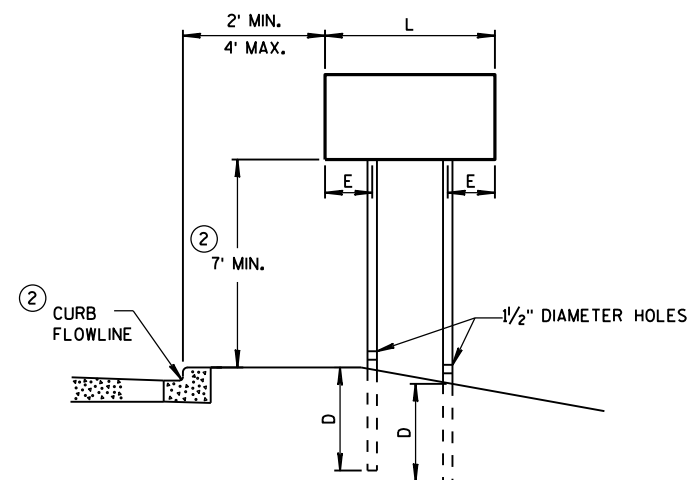
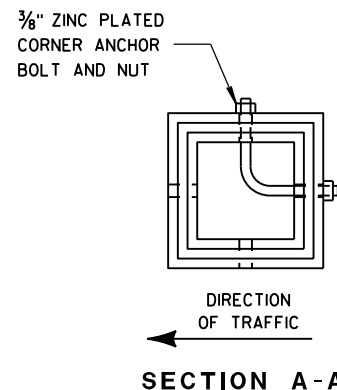


DETAIL OF TUBULAR STEEL SIGN POST

TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).
SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

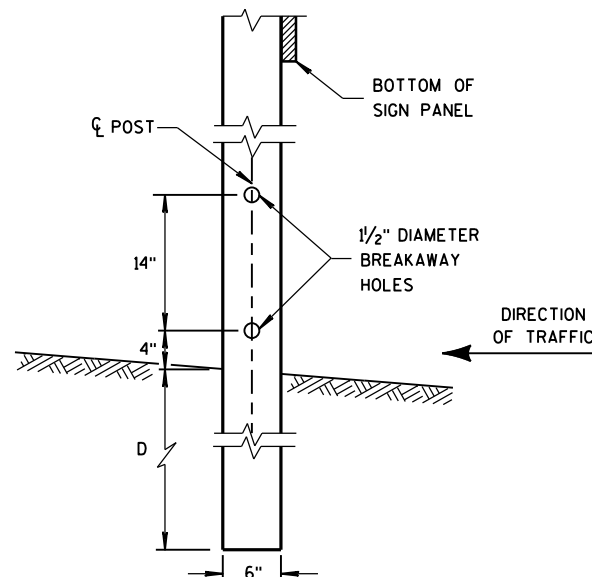


URBAN AREA

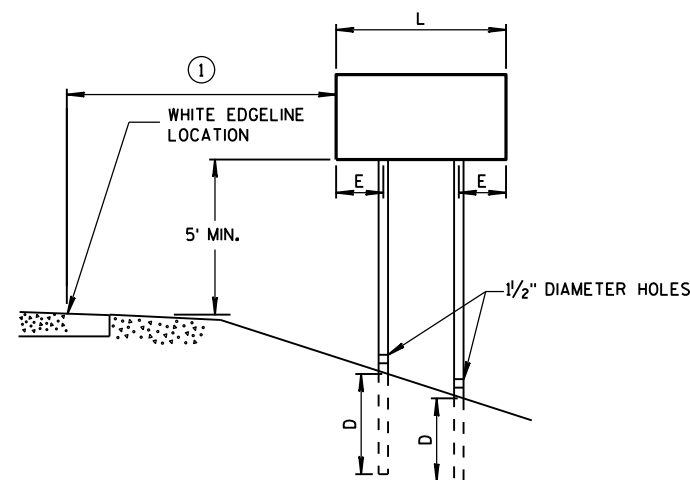
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



4" x 6" WOOD POST MODIFICATION



RURAL AREA

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

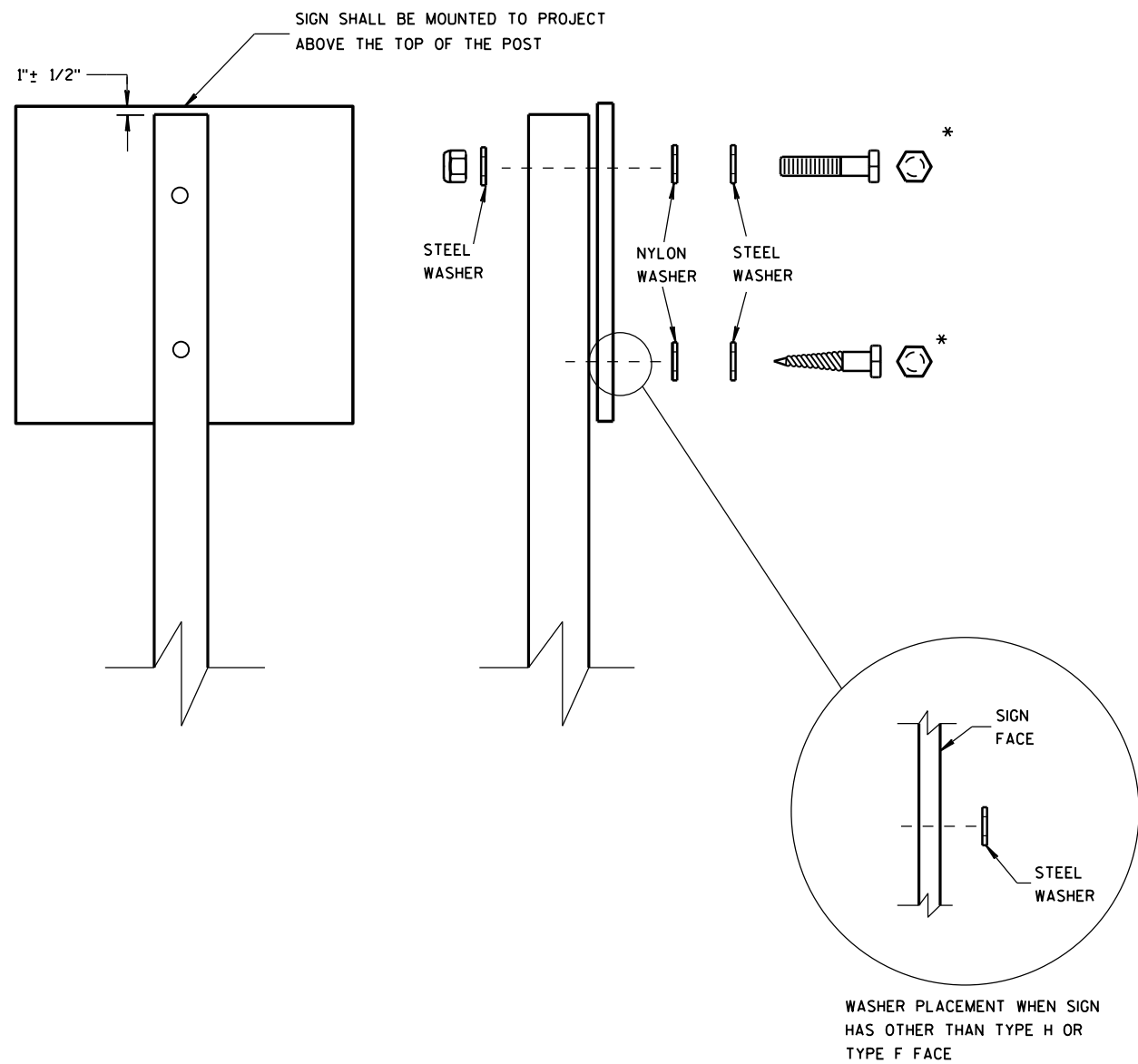
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

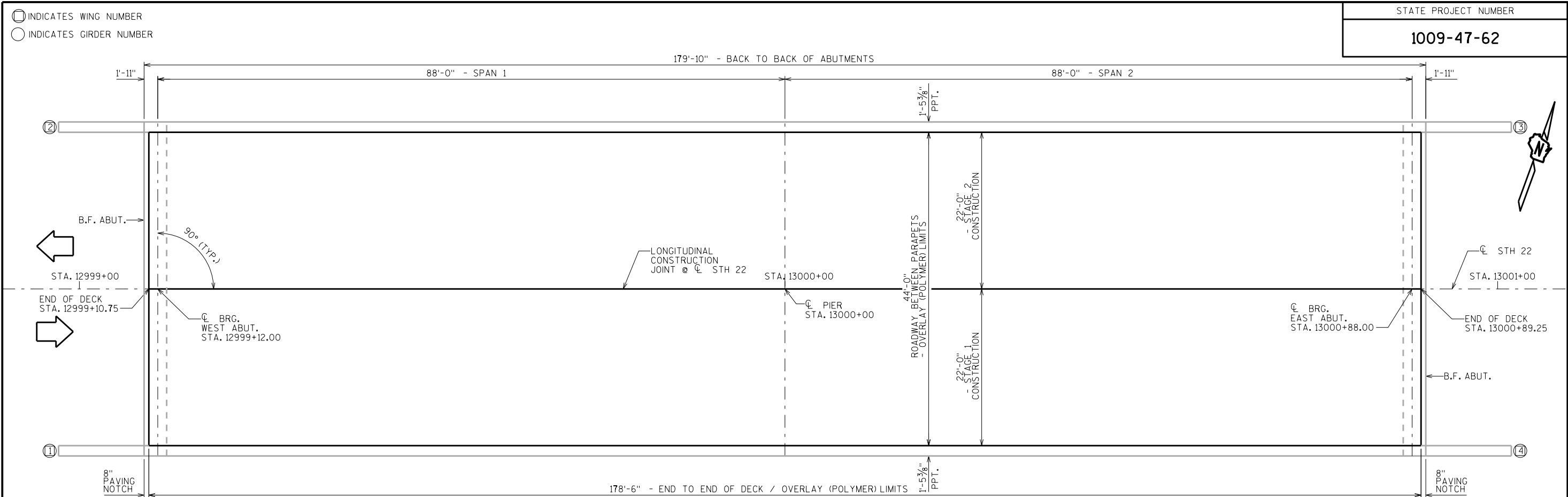
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" X 3"
 - MACHINE BOLTS - 5/16" X 6-1/2" OR 7" LENGTH W/ NUTS

- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS
 - RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 - 1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



PLAN

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-28
OPERATING RATING: HS-45
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

TRAFFIC VOLUME

STH 22
A.D.T. = 4,600 (2015)
R.D.S. = 60 M.P.H.

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	873

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".



LIST OF DRAWINGS

1. POLYMER OVERLAY
STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
ACCEPTED		2/15/19	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-58-111			
STH 22 OVER EMBARRASS RIVER			
COUNTY	SHAWANO	TOWN	BELLE PLAINE
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH
POLYMER OVERLAY			PLANS CK'D. JJS
SHEET 1 OF 1			



BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	2,077

DESIGN LOADING: HL-93
INVENTORY RATING FACTOR: RF = 1.14
OPERATING RATING FACTOR: RF = 1.48
WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 250 KIPS

DECK SURFACE PREPARATION IS INCLUDED
IN THE BID ITEM "POLYMER OVERLAY".

A.D.T. = 7,700 (2015)
R.D.S. = 60 M.P.H.



1. POLYMER OVERLAY

DATE: OCT. 2018

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-29
OPERATING RATING: HS-49
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 47
A.D.T. = 4,900 (2015)
R.D.S. = 60 M.P.H.

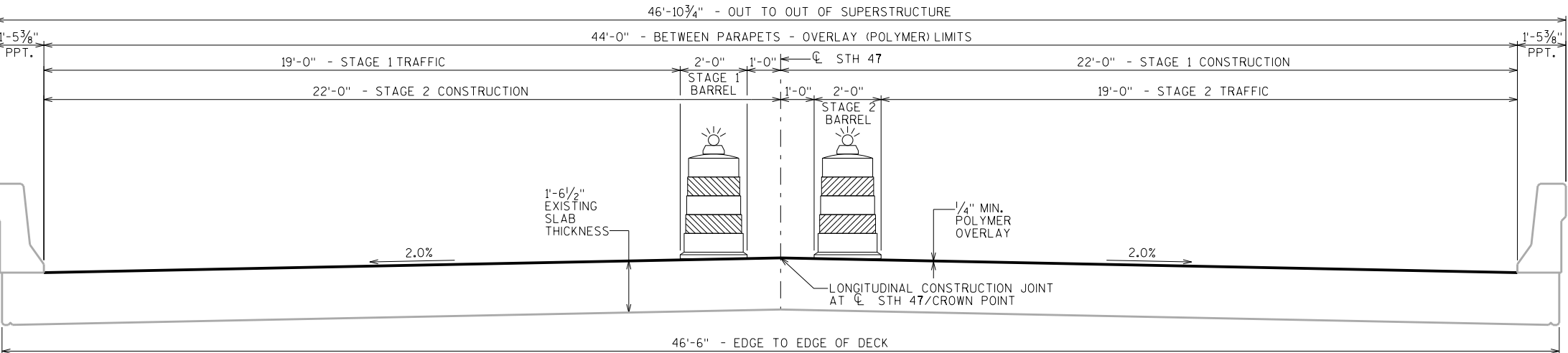
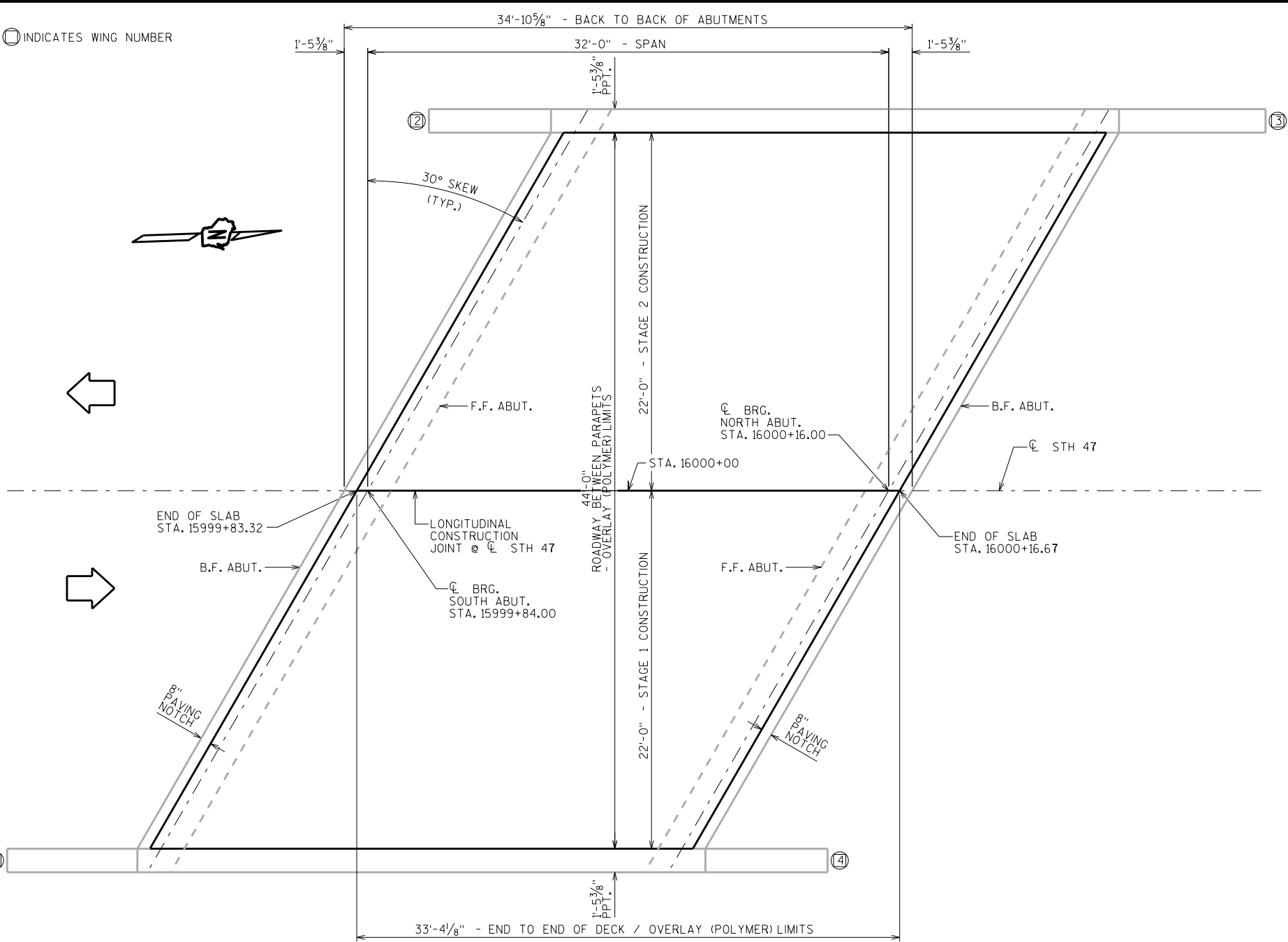


TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509,5100,S	POLYMER OVERLAY	SY	163

STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

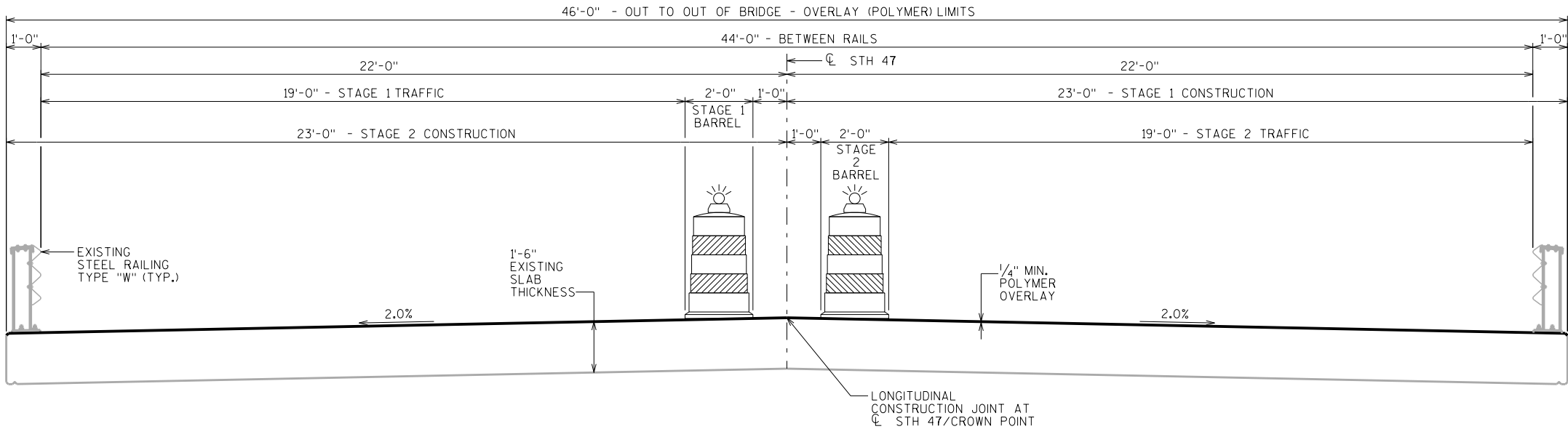
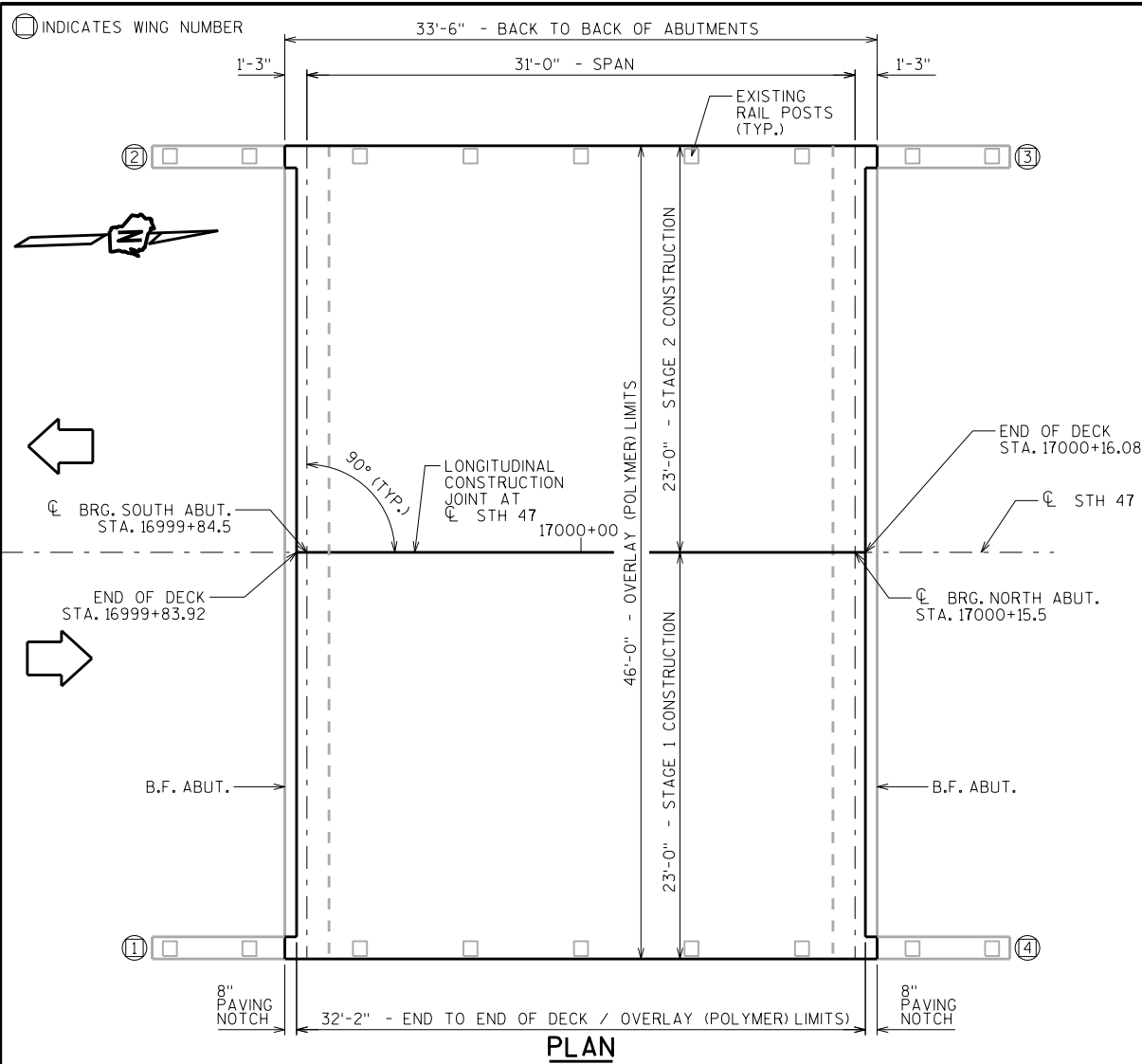
NO.	DATE	REVISION	BY
<div><div></div><div>BUREAU OF STRUCTURES</div></div>			
ACCEPTED <i>William C. Diehl</i>		2/15/19	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-58-121			
STH 47 OVER BRANCH HERMAN CREEK			
COUNTY	SHAWANO	TOWN	LESSOR
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D.RATINGS	DRAWN BY JPH
PLANS CK'D. JJS			
POLYMER OVERLAY			SHEET 1 OF 1



TYPICAL CROSS SECTION THRU BRIDGE LOOKING NORTH

LIST OF DRAWINGS

1. POLYMER OVERLAY



CROSS SECTION THRU BRIDGE LOOKING NORTH

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-28
OPERATING RATING: HS-46
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 47
A.D.T. = 4,900 (2015)
R.D.S. = 60 M.P.H.




TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	164

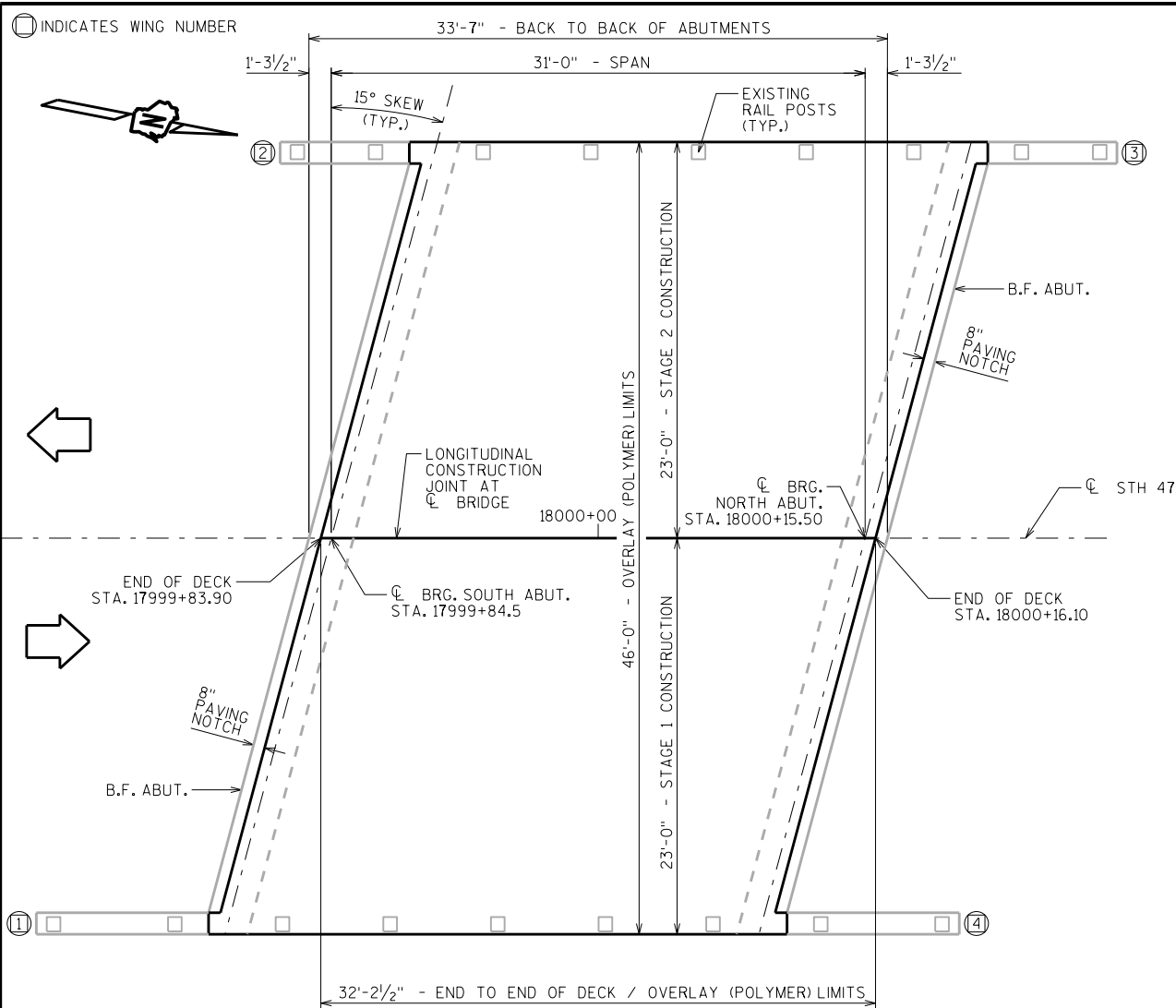
STATE PROJECT NUMBER
1009-47-62

STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

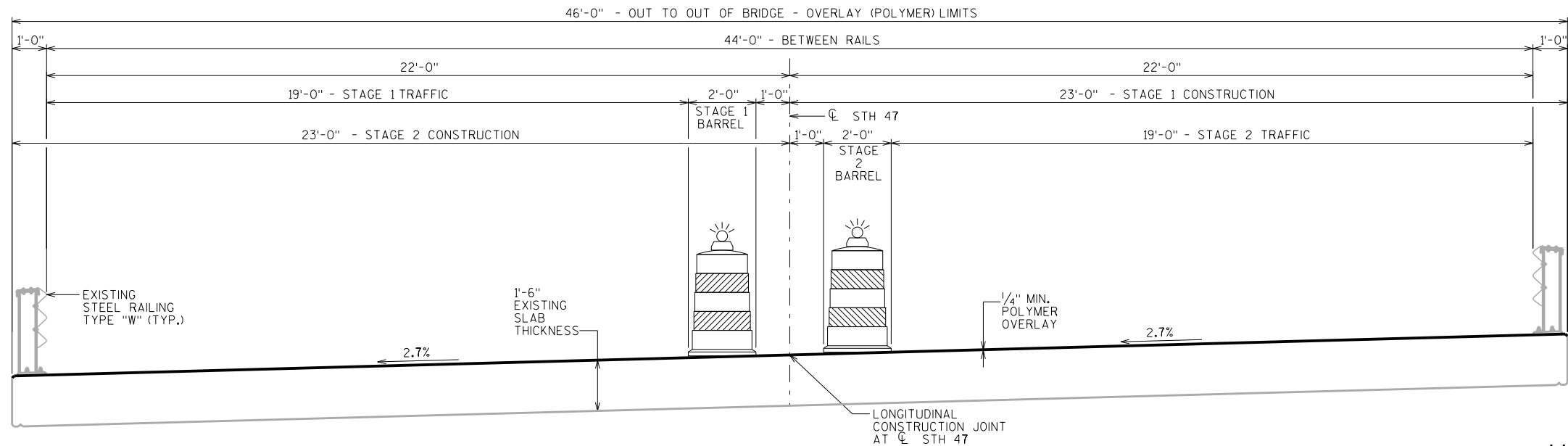
NO.	DATE	REVISION	BY
 BUREAU OF STRUCTURES			
ACCEPTED <i>William C. Diekmann</i> 2/15/19		CHIEF STRUCTURES DESIGN ENGINEER DATE	
STRUCTURE B-58-122			
STH 47 OVER MINK CREEK			
COUNTY	SHAWANO	TOWN	LESSOR
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH PLANS CK'D. JJS
POLYMER OVERLAY			SHEET 1 OF 1

LIST OF DRAWINGS

1. POLYMER OVERLAY



PLAN



CROSS SECTION THRU BRIDGE LOOKING NORTH

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-28
OPERATING RATING: HS-47
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 47
A.D.T. = 4,900 (2015)
R.D.S. = 60 M.P.H.




TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	165

STATE PROJECT NUMBER
1009-47-62

STRUCTURE DESIGN CONTACTS:
JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.	DATE	REVISION	BY
 BUREAU OF STRUCTURES			
ACCEPTED <i>William C. Decker</i>		2/15/19	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-58-123			
STH 47 OVER SLAB CITY CREEK			
COUNTY	SHAWANO	TOWN	HARTLAND
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	JJS	DESIGNED CK'D. RATINGS	DRAWN BY JPH
PLANS CK'D. JJS		SHEET 1 OF 1	
POLYMER OVERLAY			

LIST OF DRAWINGS

1. POLYMER OVERLAY

DESIGN DATA

LIVE LOAD:
INVENTORY RATING: HS-26
OPERATING RATING: HS-44
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
DECK SURFACE PREPARATION IS INCLUDED IN THE
BID ITEM "POLYMER OVERLAY".

TRAFFIC VOLUME

STH 156
A.D.T. = 1,300 (2015)
R.D.S. = 60 M.P.H.



TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	187

STRUCTURE DESIGN CONTACTS:

JOHN SENDOR (608) 266-5163
AARON BONK (608) 261-0261

NO.

DATE

REVISION

BY

BUREAU OF STRUCTURES

ACCEPTED *William C. Dehner* **3/18/19**
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-58-124

STH 156 OVER SHIOC RIVER

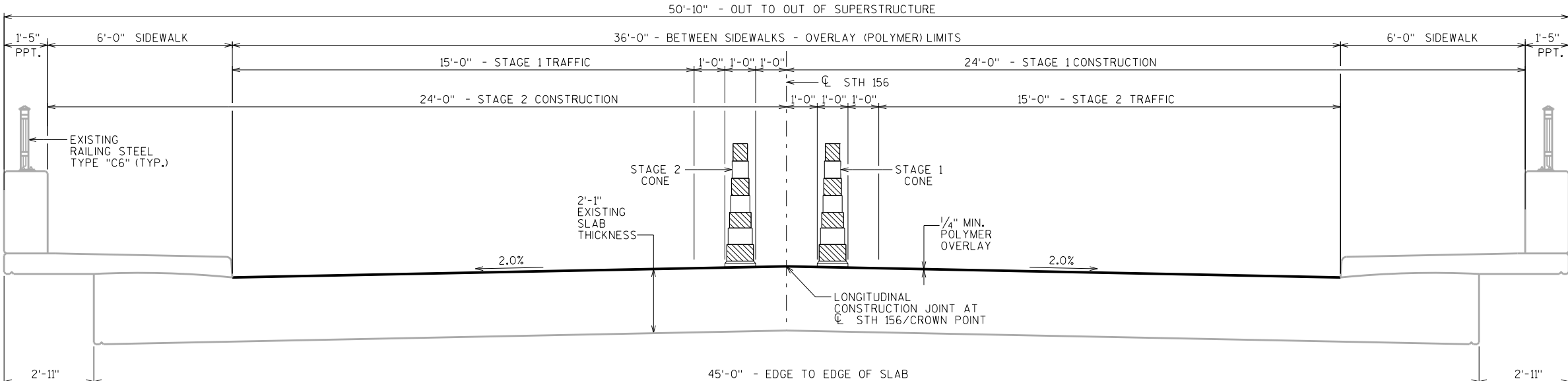
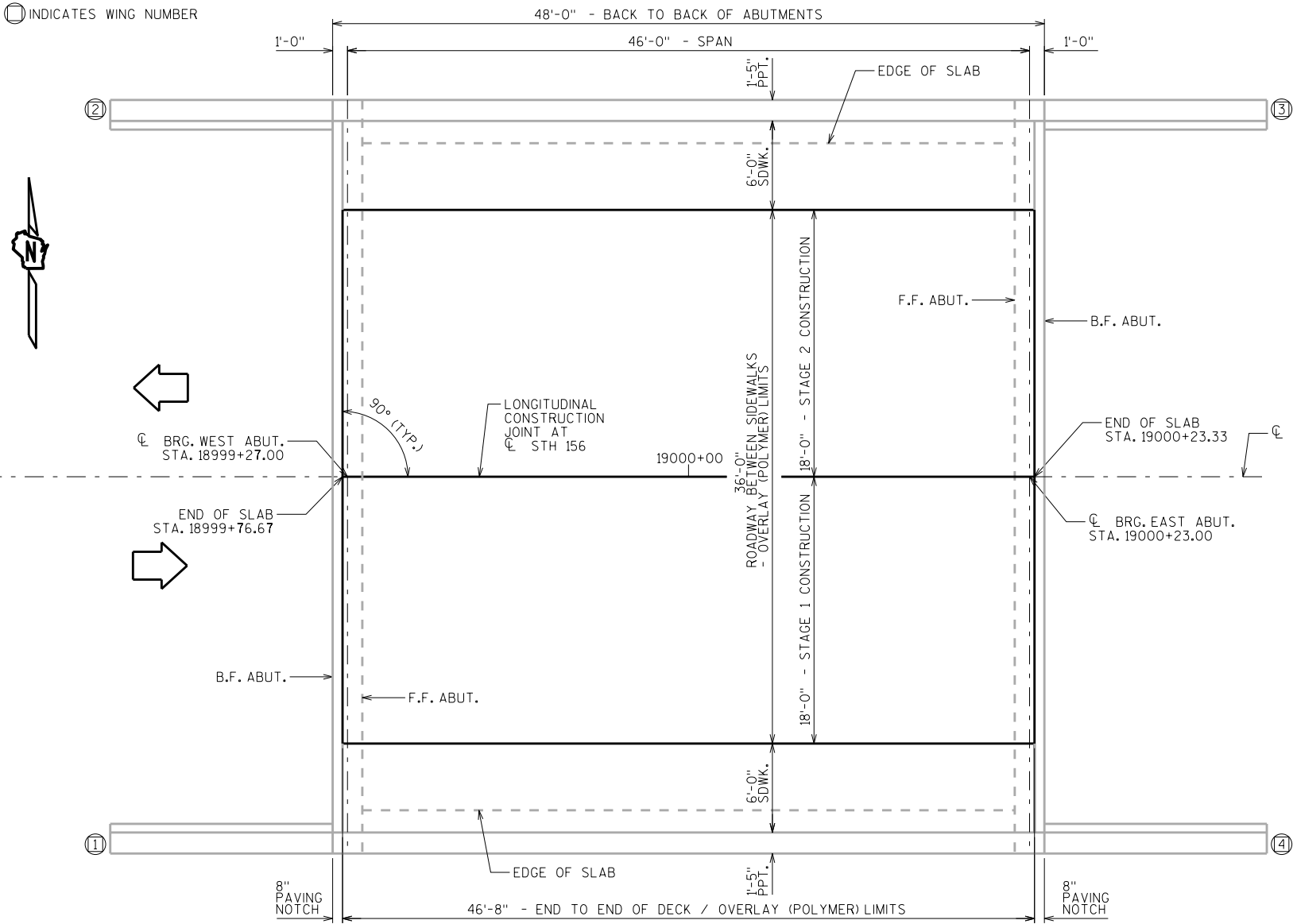
COUNTY SHAWANO TOWN NAVARINO

DESIGN SPEC. REHABILITATION N/A

DESIGNED BY JJS DESIGNED CK'D. RATINGS DRAWN BY JPH PLANS CK'D. JJS

POLYMER OVERLAY

SHEET 1 OF 1



LIST OF DRAWINGS

1. POLYMER OVERLAY

SCALE = 5.00



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>