



LIST OF STANDARD ABBREVIATIONS

ABUT.	ABUTMENT
AGG.	AGGREGATE
AH.	AHEAD
APPROX.	APPROXIMATE
A.E.W.	APRON ENDWALL
ASPH.	ASPHALTIC
A.D.T.	AVERAGE DAILY TRAFFIC
AZ.	AZIMUTH
BK.	BACK
BEG.	BEGIN
B.M.	BENCH MARK
C/L	CENTER LINE
CONC.	CONCRETE
CONST.	CONSTRUCTION
CO.	COUNTY
C.T.H.	COUNTY TRUNK HIGHWAY
X-SEC.	CROSS SECTION
CR.	CRUSHED
CFS	CUBIC FEET/SECOND
C.Y., CU. YD.	CUBIC YARD
CULV.	CULVERT
C.P.	CULVERT PIPE
D.O.T.	DEPARTMENT OF TRANSPORTATION
D.H.V.	DESIGN HOUR VOLUME
DIA.	DIAMETER
D.	DIRECTIONAL DISTRIBUTION
DISCH. OR DIS.	DISCHARGE
EA.	EACH
ELECT.	ELECTRIC
EL. OR ELEV.	ELEVATION
EMB.	EMBANKMENT
E.B.S.	EXCAVATION BELOW SUBGRADE
EXIST.	EXISTING
FERT.	FERTILIZE
F.E.	FIELD ENTRANCE
FIN.	FINISHED
FT.	FOOT
F.L.	FLOW LINE
GA.	GAUGE
HORIZ.	HORIZONTAL
CWT.	HUNDREDWEIGHT
INL.	INLET
LT.	LEFT
L.H.F.	LEFT-HAND FORWARD
LIN.	LINEAR
LIN. FT.	LINEAR FOOT
L.S.	LUMP SUM
MAX.	MAXIMUM
MI.	MILE
MISC.	MISCELLANEOUS
N.E.	NORTH EAST
N.W.	NORTH WEST
PAV'T	PAVEMENT
P.C.	POINT OF CURVATURE
P.I.	POINT OF INTERSECTION
P.T.	POINT OF TANGENCY
P.O.T.	POINT ON TANGENT
LB.	POUND
P.E.	PRIVATE ENTRANCE
PROJ.	PROJECT
R.	RANGE
REQ'D	REQUIRED
RT.	RIGHT
R.H.F.	RIGHT-HAND FORWARD
R/W	RIGHT OF WAY
RD.	ROAD
SHR.	SHRINKAGE
SL.	SLOPE
STD.	STANDARD
S.D.D.	STANDARD DETAIL DRAWINGS
S.T.H.	STATE TRUNK HIGHWAY
STA.	STATION
S.P.P.A.	STRUCTURAL PLATE PIPE ARCH
STRUCT.	STRUCTURE
SURF.	SURFACE
TEL.	TELEPHONE
TN.	TOWN
T.	TRUCKS (PERCENT OF)
UNCL.	UNCLASSIFIED
U.G.	UNDERGROUND
V.	VELOCITY OR DESIGN SPEED
V.C.	VERTICAL CURVE

UTILITIES CONTACTS

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GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS, AND WORK NOT SHOWN SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, SPECIAL PROVISIONS, STANDARD DETAILS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THERE ARE EXISTING UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA PRIOR TO BEGINNING WORK OPERATIONS TO CONFIRM ALL UTILITY LOCATIONS. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE. UTILITY FACILITIES WERE NOT FIELD LOCATED AND VERIFIED.

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WISCONSIN DOT - STORMWATER  
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DIGGERS



HOTLINE

Dial  or (800)242-8511

www.DiggersHotline.com

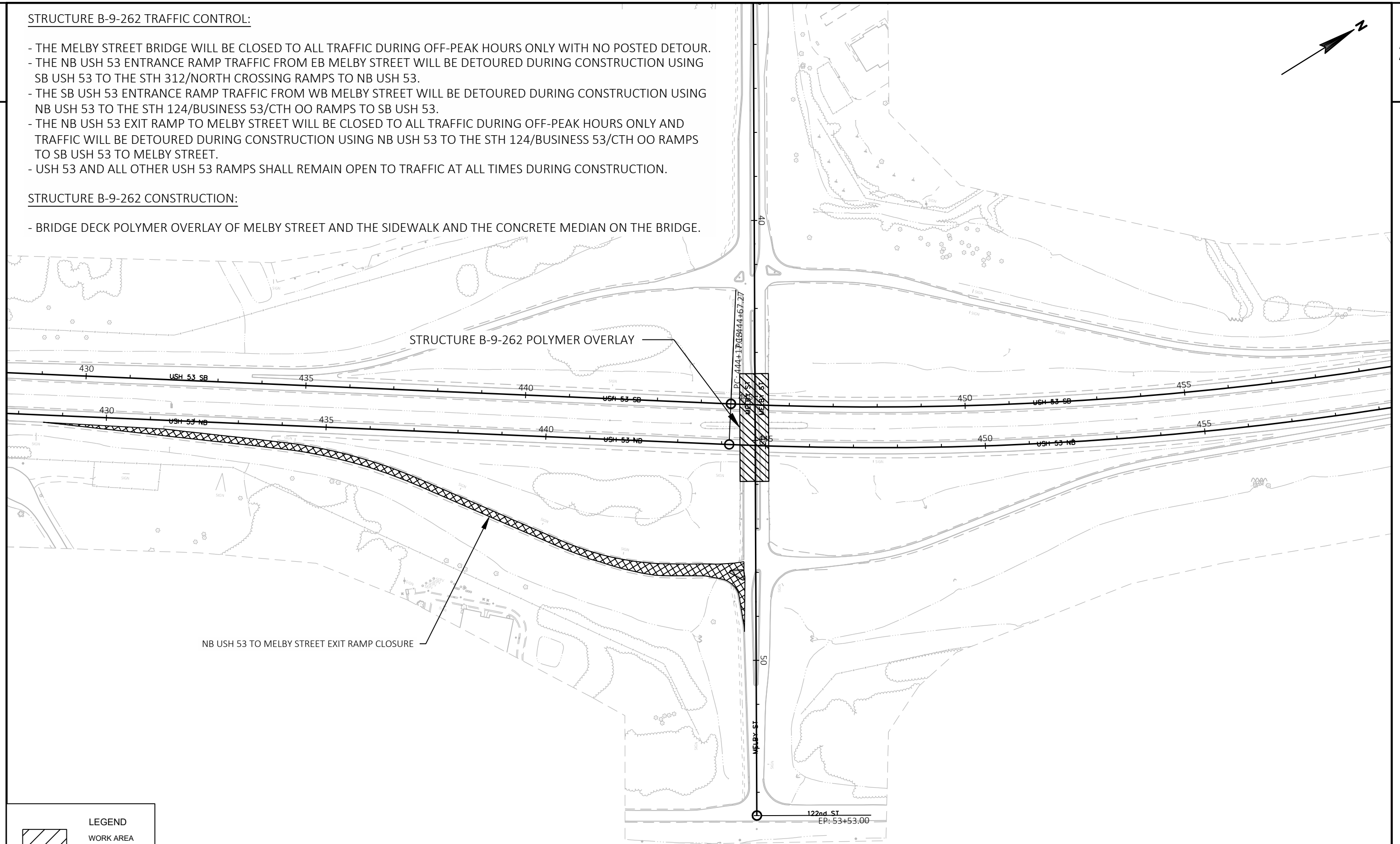


STRUCTURE B-9-262 TRAFFIC CONTROL:

- THE MELBY STREET BRIDGE WILL BE CLOSED TO ALL TRAFFIC DURING OFF-PEAK HOURS ONLY WITH NO POSTED DETOUR.
- THE NB USH 53 ENTRANCE RAMP TRAFFIC FROM EB MELBY STREET WILL BE DETOURED DURING CONSTRUCTION USING SB USH 53 TO THE STH 312/NORTH CROSSING RAMPS TO NB USH 53.
- THE SB USH 53 ENTRANCE RAMP TRAFFIC FROM WB MELBY STREET WILL BE DETOURED DURING CONSTRUCTION USING NB USH 53 TO THE STH 124/BUSINESS 53/CTH OO RAMPS TO SB USH 53.
- THE NB USH 53 EXIT RAMP TO MELBY STREET WILL BE CLOSED TO ALL TRAFFIC DURING OFF-PEAK HOURS ONLY AND TRAFFIC WILL BE DETOURED DURING CONSTRUCTION USING NB USH 53 TO THE STH 124/BUSINESS 53/CTH OO RAMPS TO SB USH 53 TO MELBY STREET.
- USH 53 AND ALL OTHER USH 53 RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-9-262 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF MELBY STREET AND THE SIDEWALK AND THE CONCRETE MEDIAN ON THE BRIDGE.



LEGEND  
WORK AREA

PROJECT NO: 1190-02-64

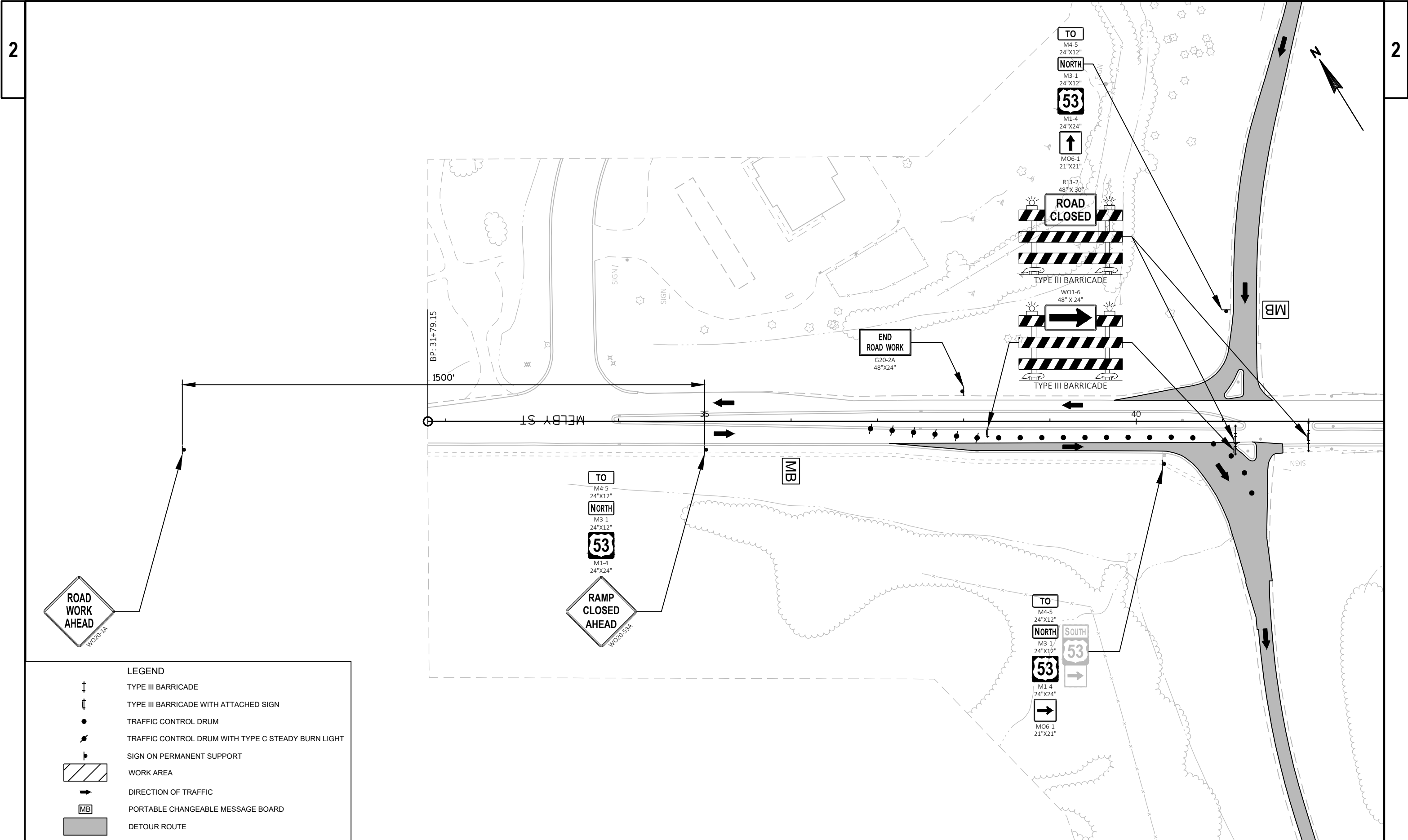
HWY: USH 53

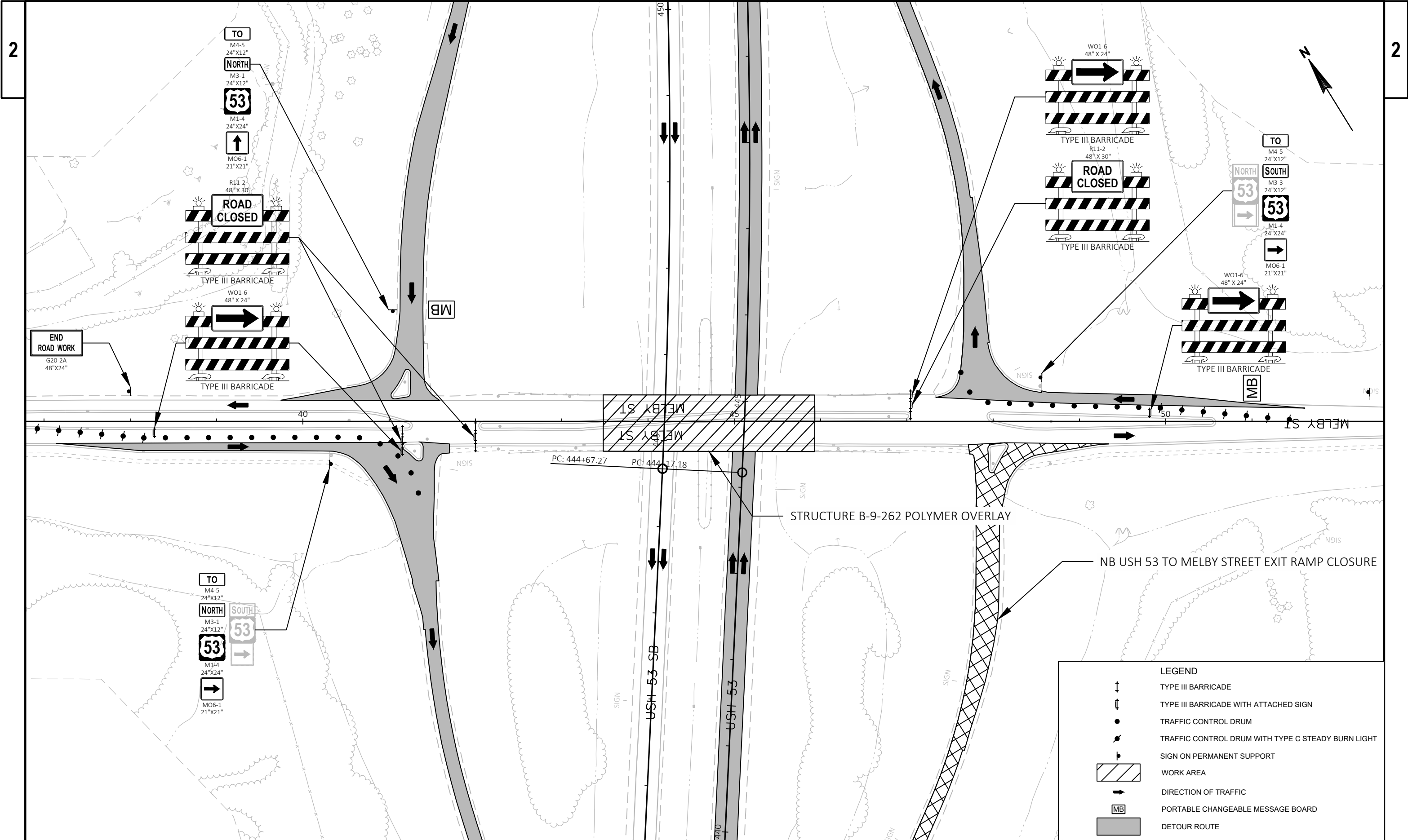
COUNTY: CHIPPEWA

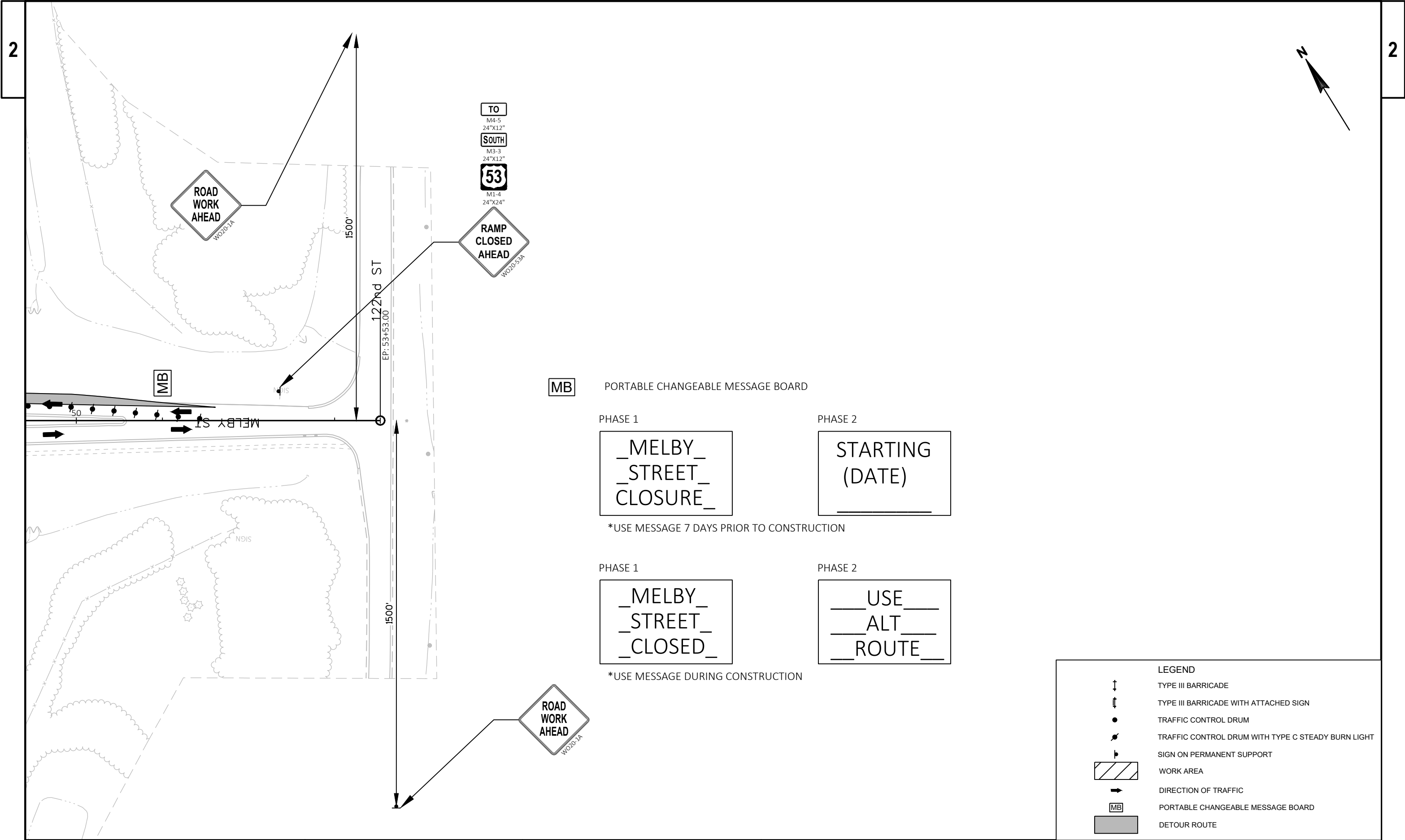
TRAFFIC CONTROL - OVERVIEW - B-9-262 MELBY STREET

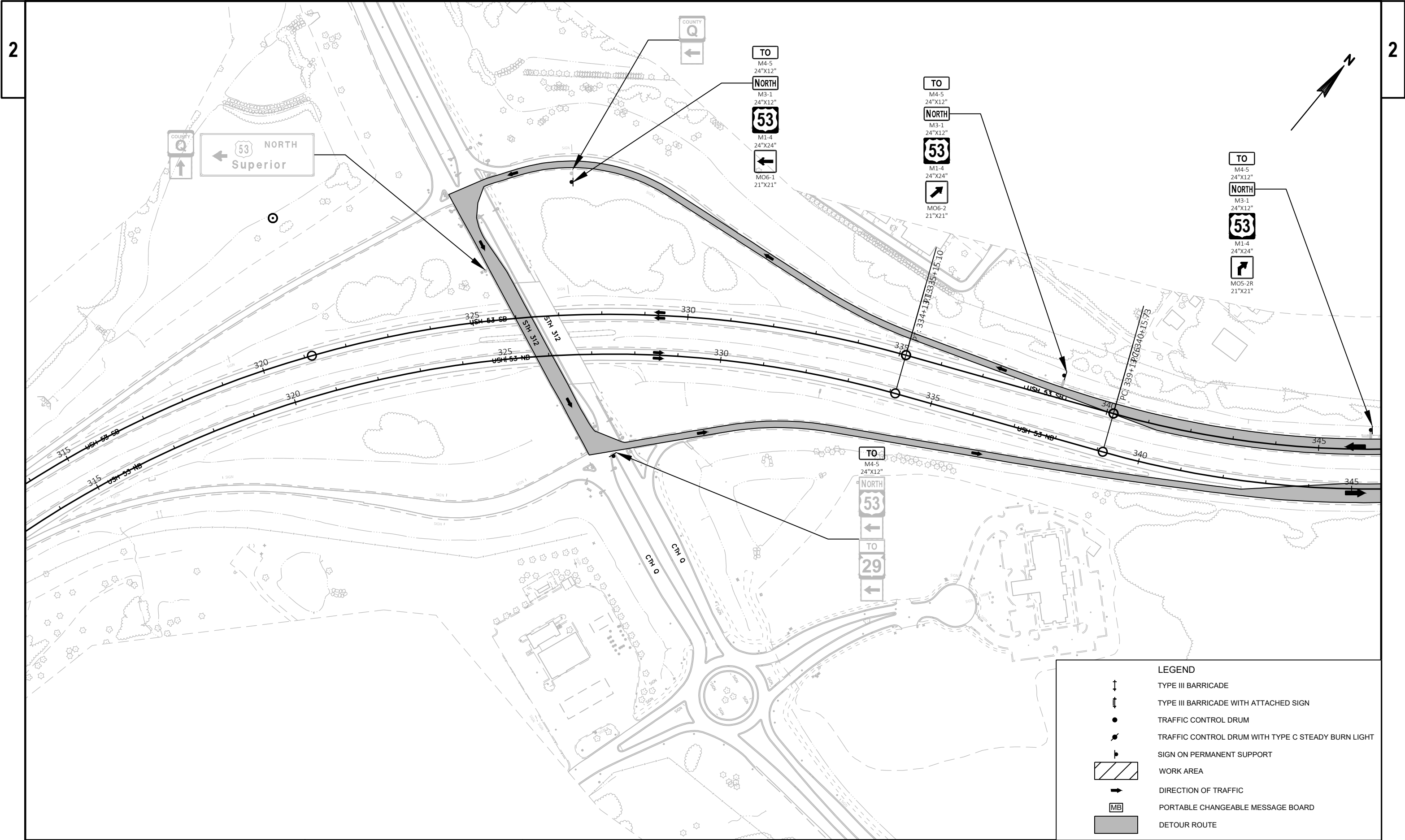
SHEET

E

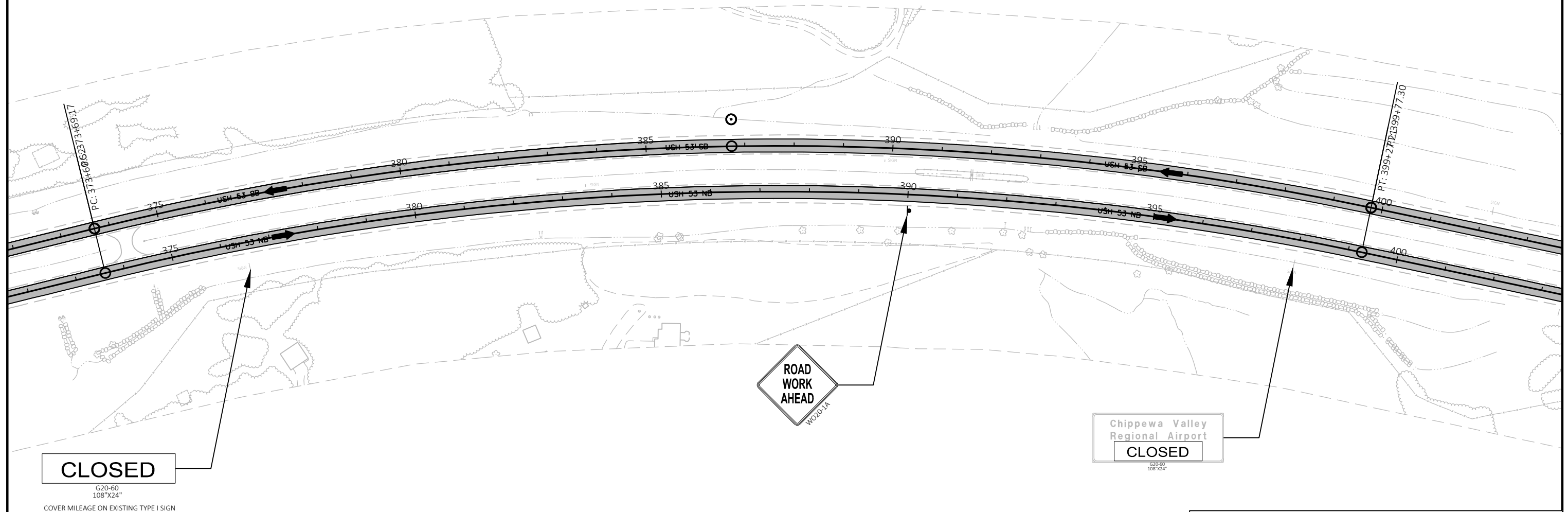


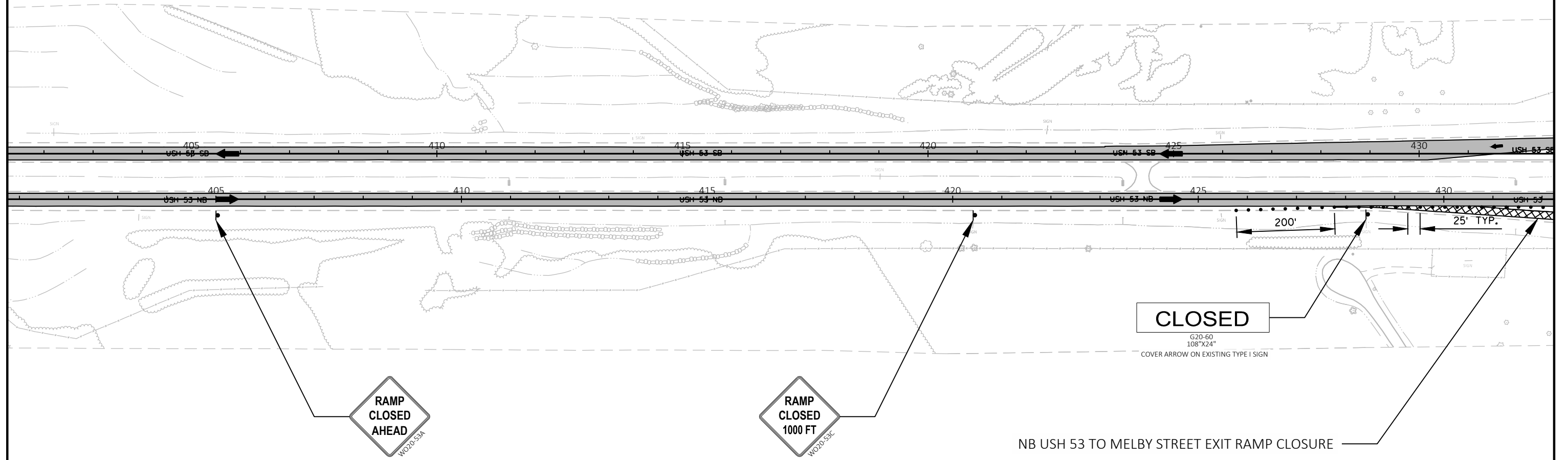
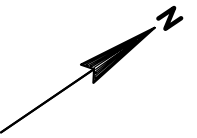






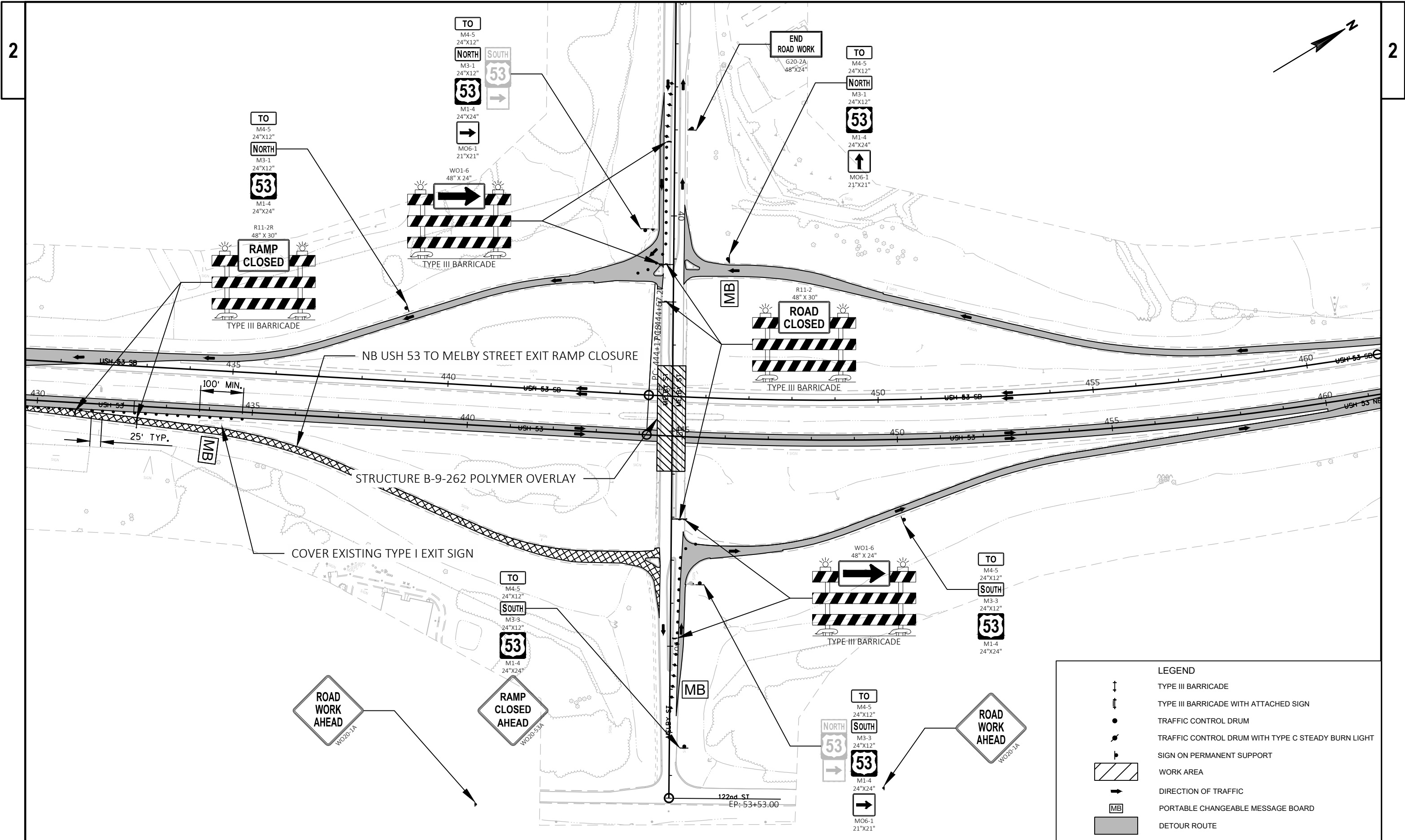


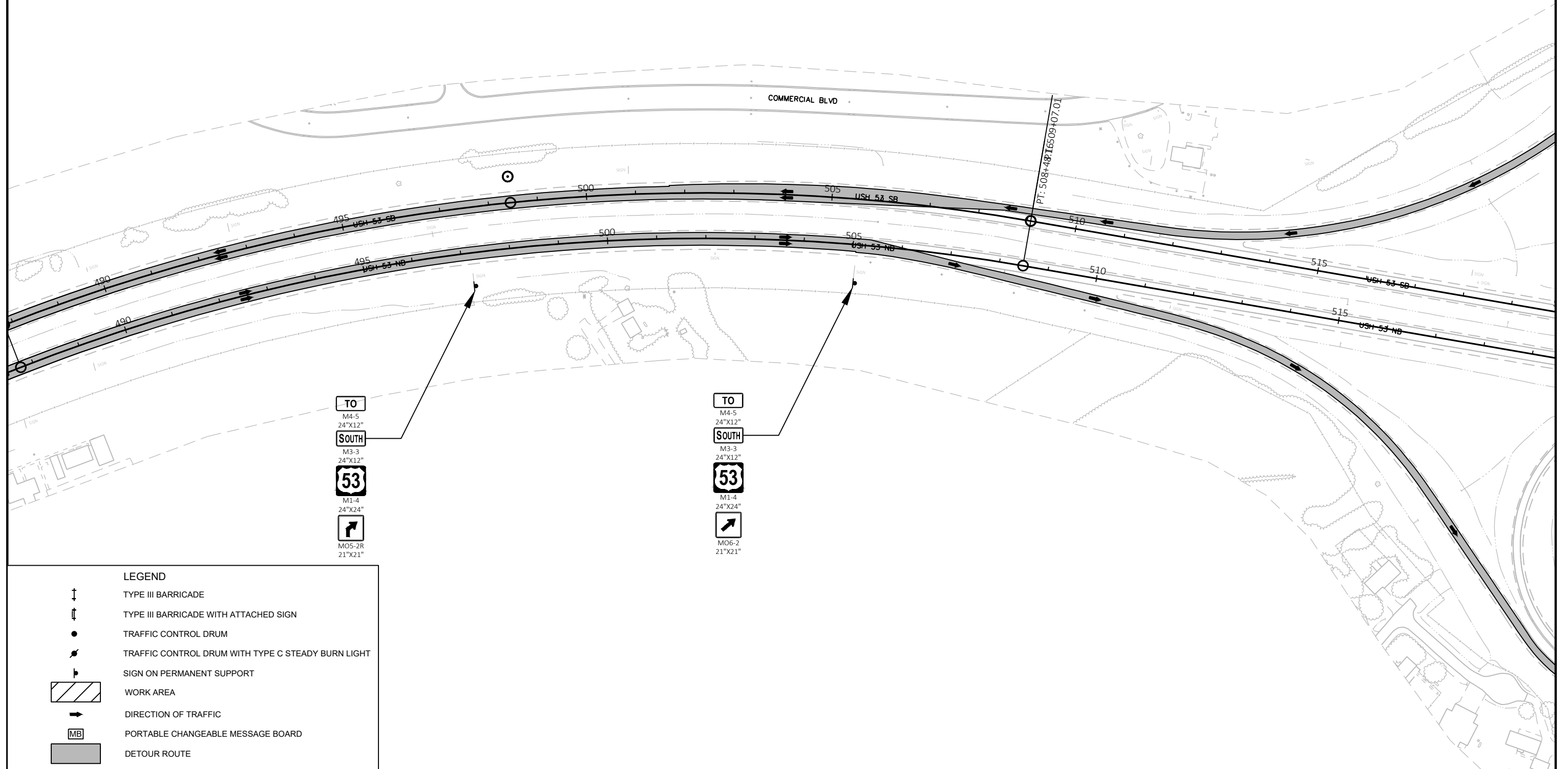
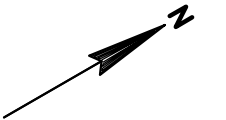


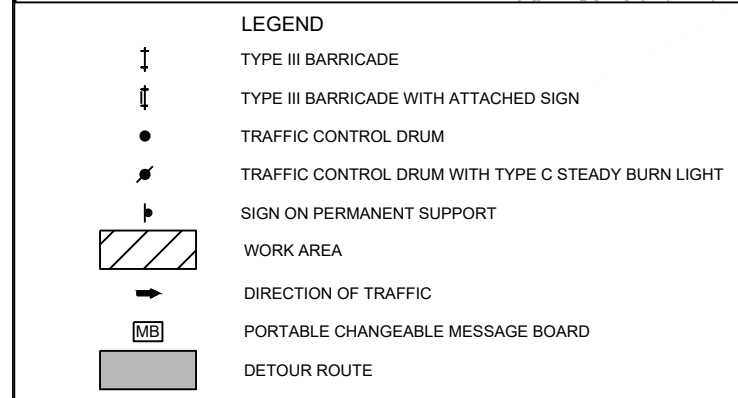


**LEGEND**

	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	WORK AREA
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD
	DETOUR ROUTE







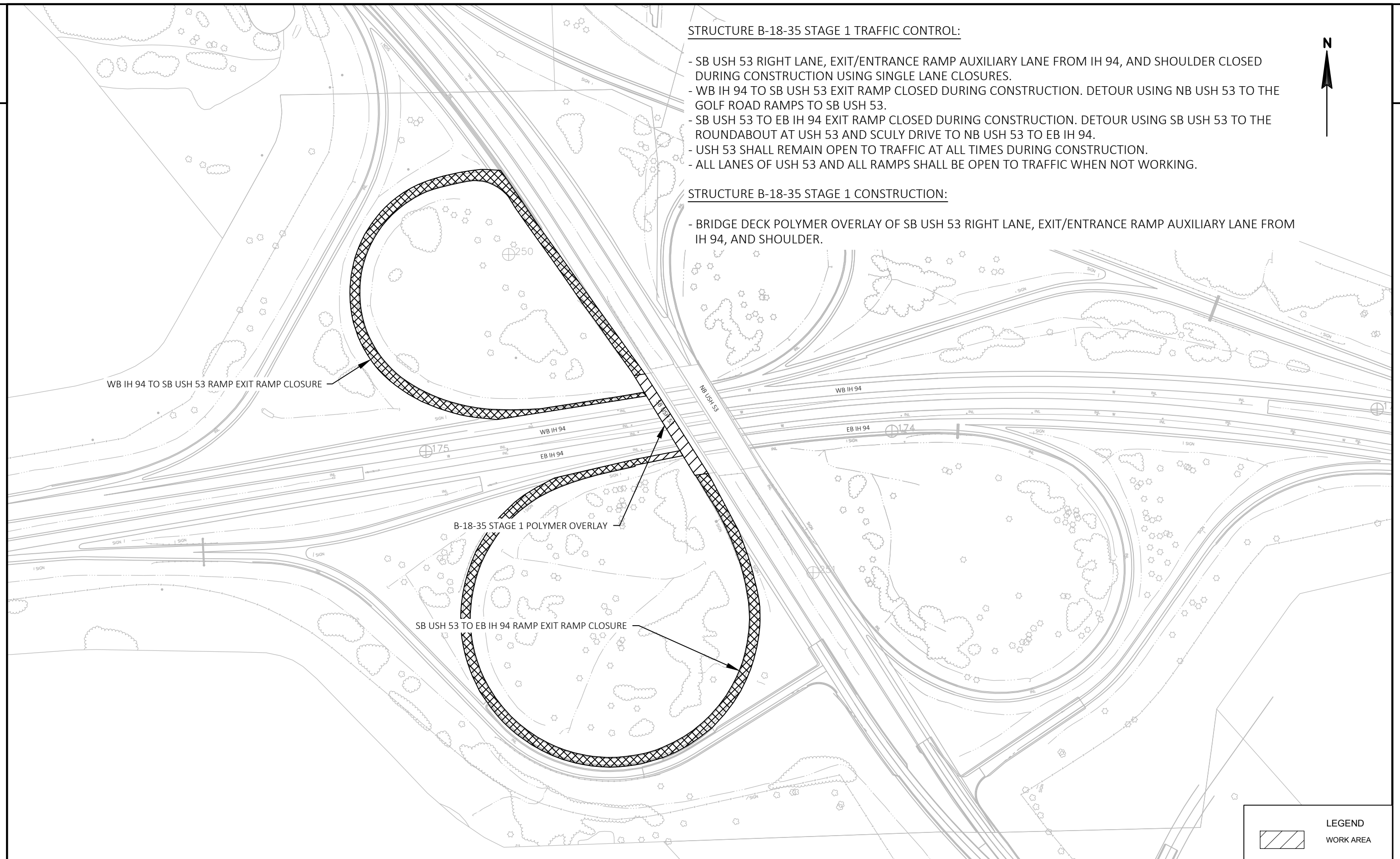


**STRUCTURE B-18-35 STAGE 1 TRAFFIC CONTROL:**

- SB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- WB IH 94 TO SB USH 53 EXIT RAMP CLOSED DURING CONSTRUCTION. DETOUR USING NB USH 53 TO THE GOLF ROAD RAMPS TO SB USH 53.
- SB USH 53 TO EB IH 94 EXIT RAMP CLOSED DURING CONSTRUCTION. DETOUR USING SB USH 53 TO THE ROUNDABOUT AT USH 53 AND SCULY DRIVE TO NB USH 53 TO EB IH 94.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

**STRUCTURE B-18-35 STAGE 1 CONSTRUCTION:**

- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

PLAN: TRAFFIC CONTROL - STAGE 1 OVERVIEW - B-18-35

SHEET

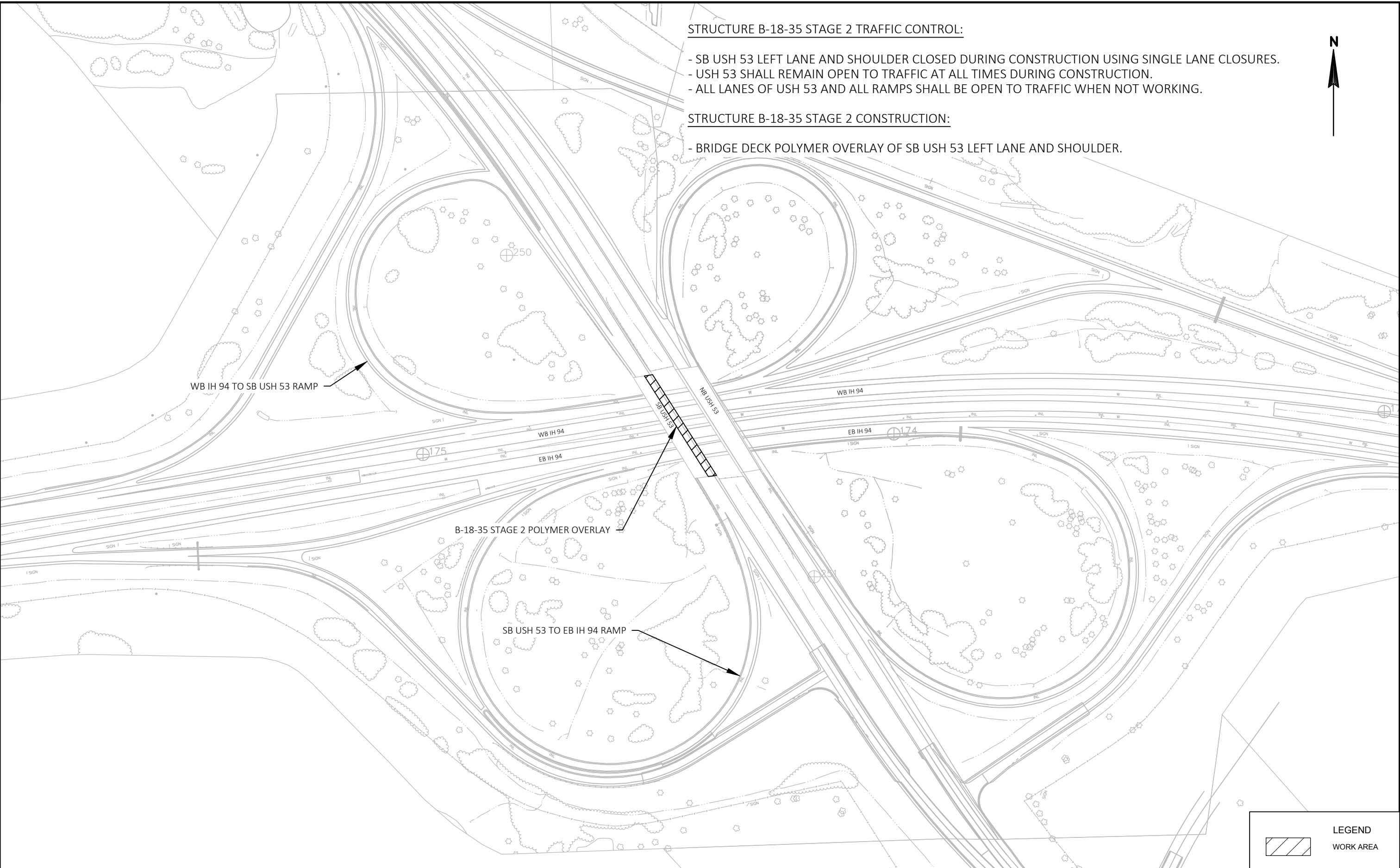
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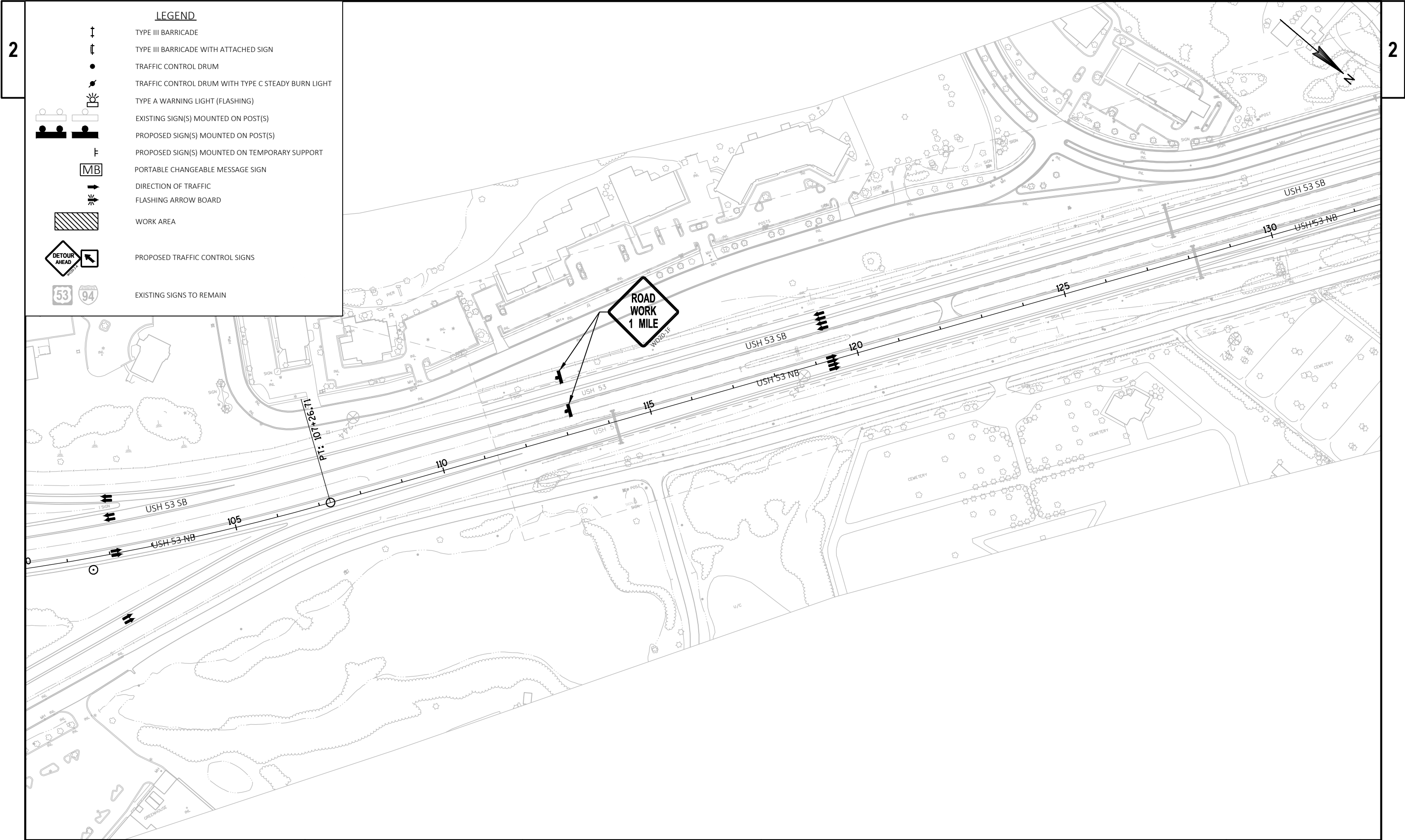
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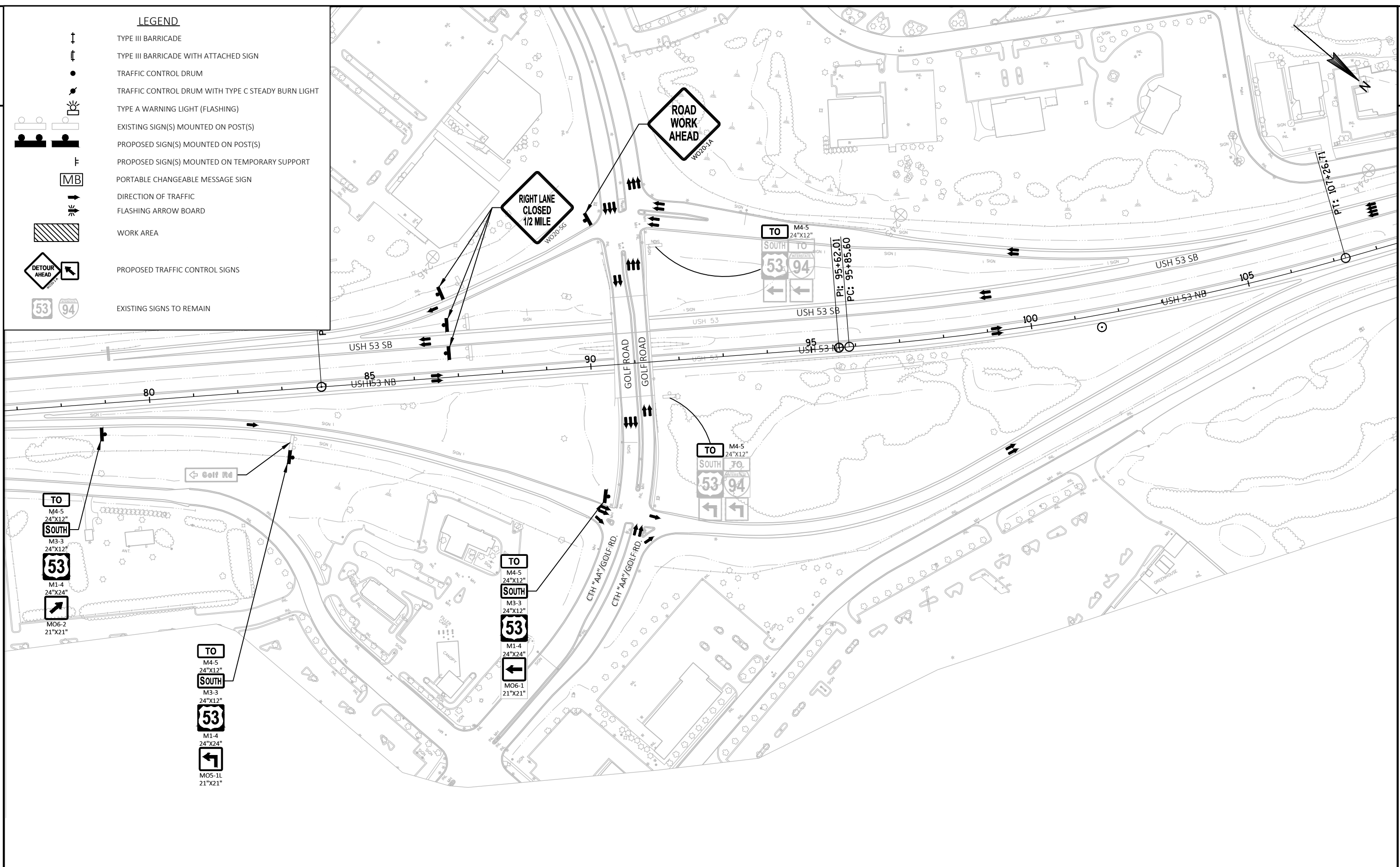
- SB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-35 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 LEFT LANE AND SHOULDER.

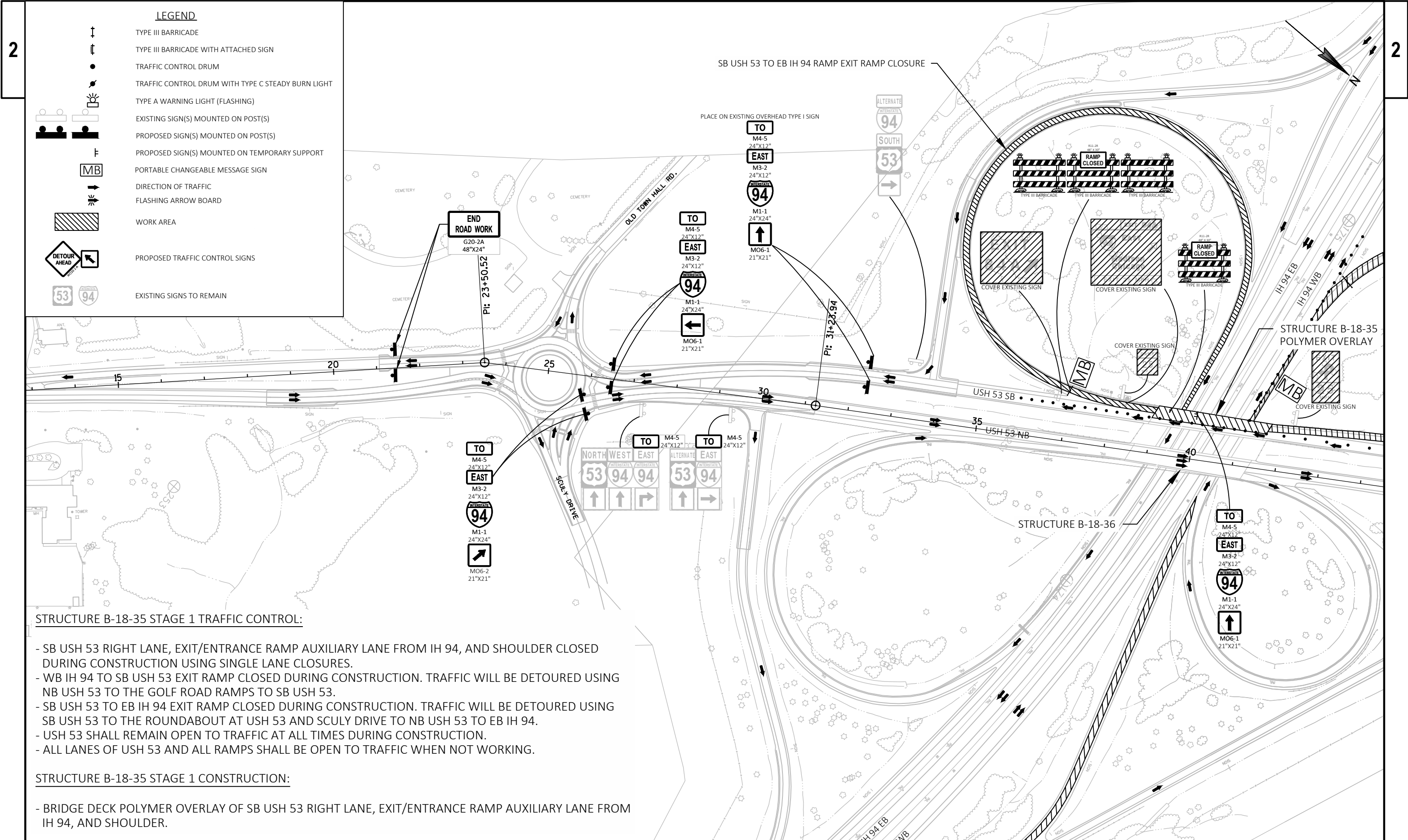












STRUCTURE B-18-35 STAGE 1 TRAFFIC CONTROL:

- SB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- WB IH 94 TO SB USH 53 EXIT RAMP CLOSED DURING CONSTRUCTION. TRAFFIC WILL BE DETOURED USING NB USH 53 TO THE GOLF ROAD RAMPS TO SB USH 53.
- SB USH 53 TO EB IH 94 EXIT RAMP CLOSED DURING CONSTRUCTION. TRAFFIC WILL BE DETOURED USING SB USH 53 TO THE ROUNDABOUT AT USH 53 AND SCULY DRIVE TO NB USH 53 TO EB IH 94.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-35 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER.

PROJECT NO: 1190-02-64

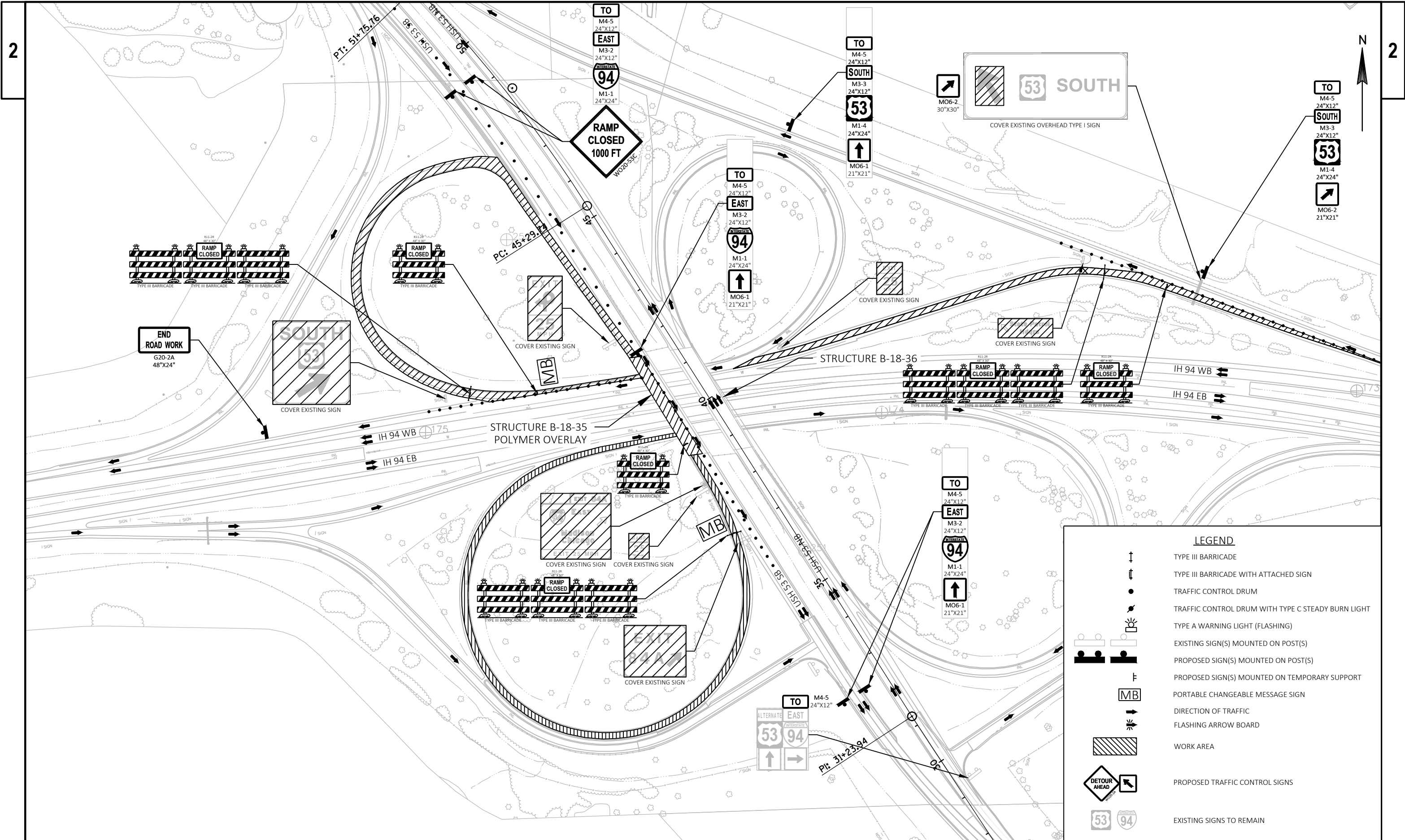
HWY: USH 53

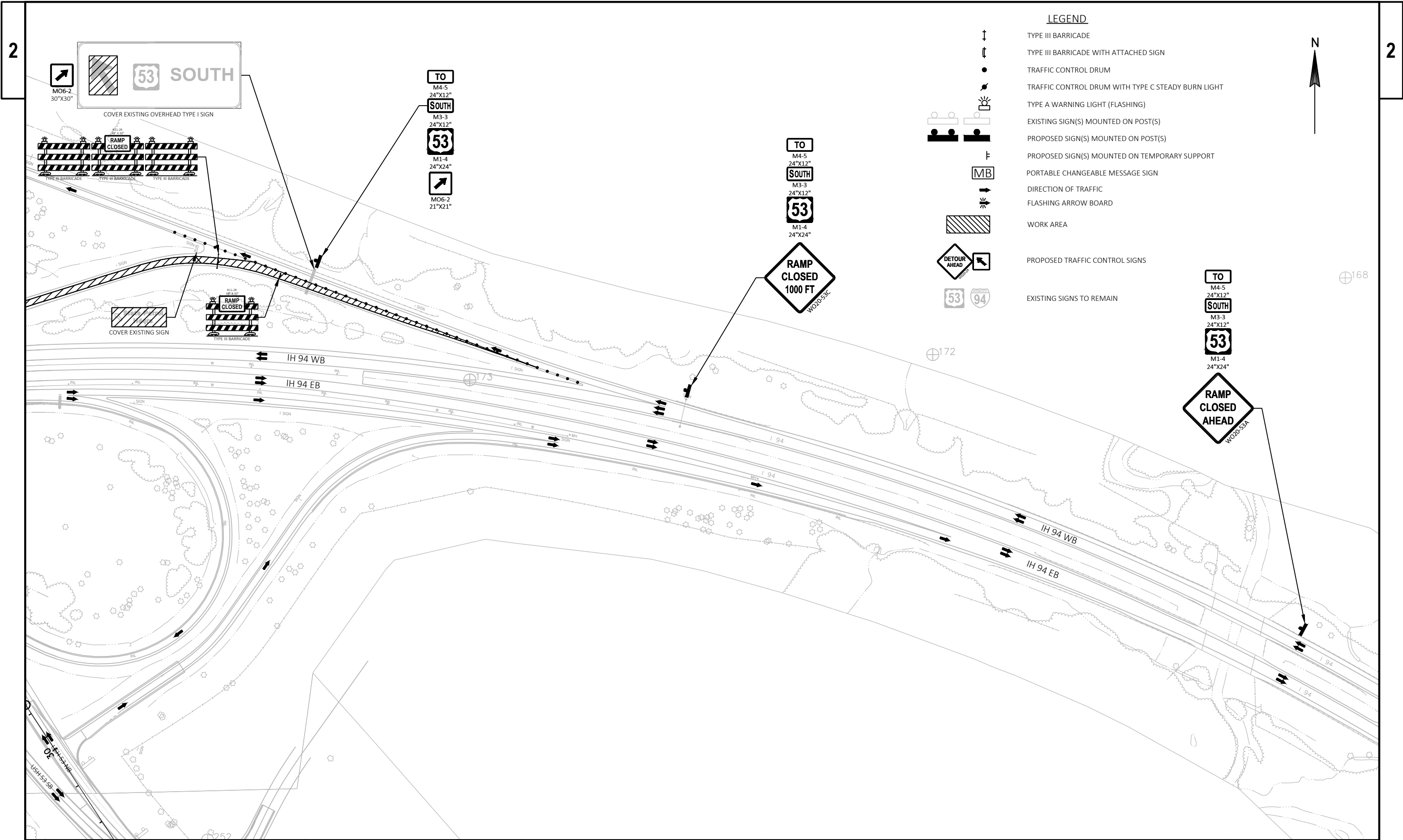
COUNTY: EAU CLAIRE

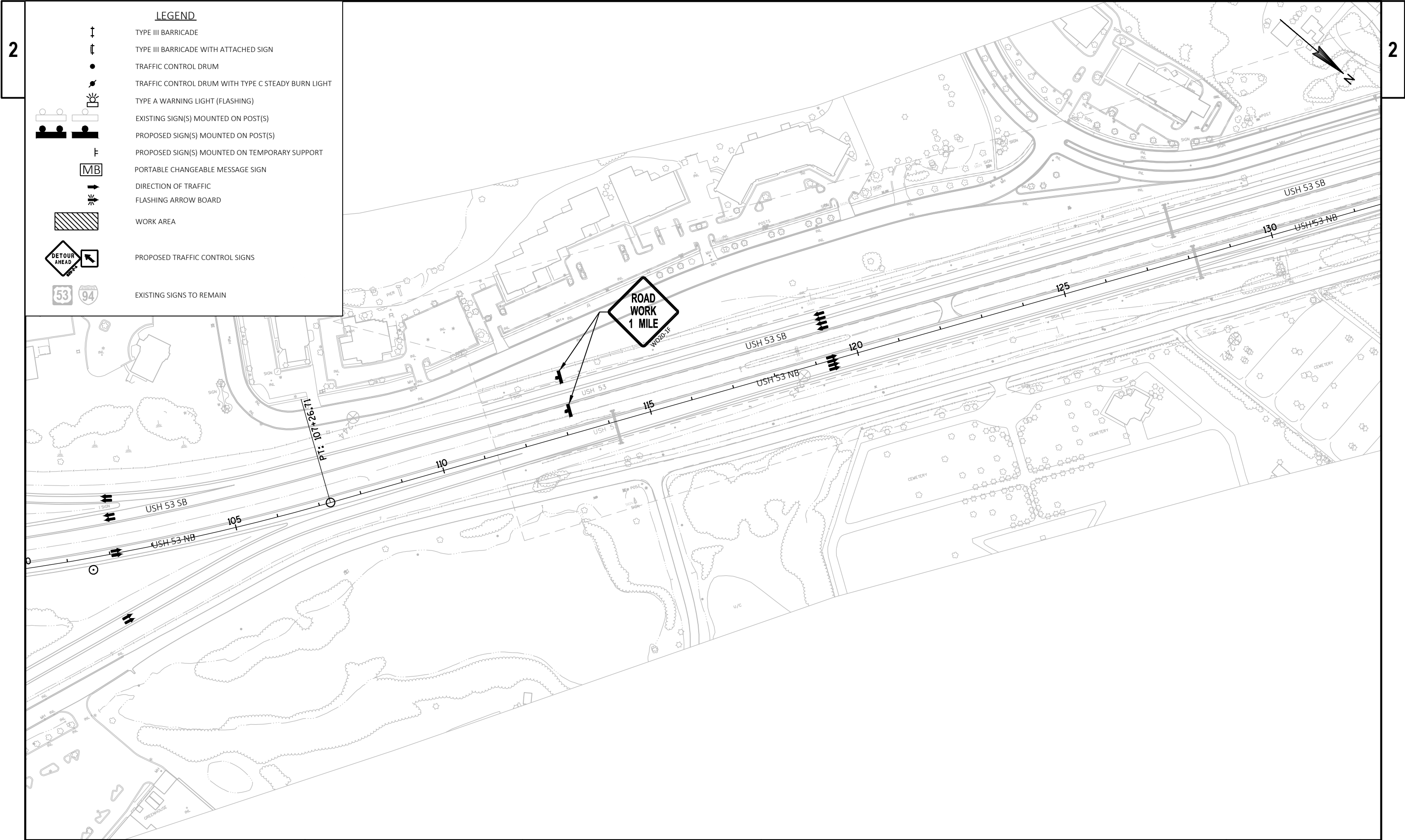
TRAFFIC CONTROL - B-18-35 STAGE 1 DETOUR PLAN

SHEET

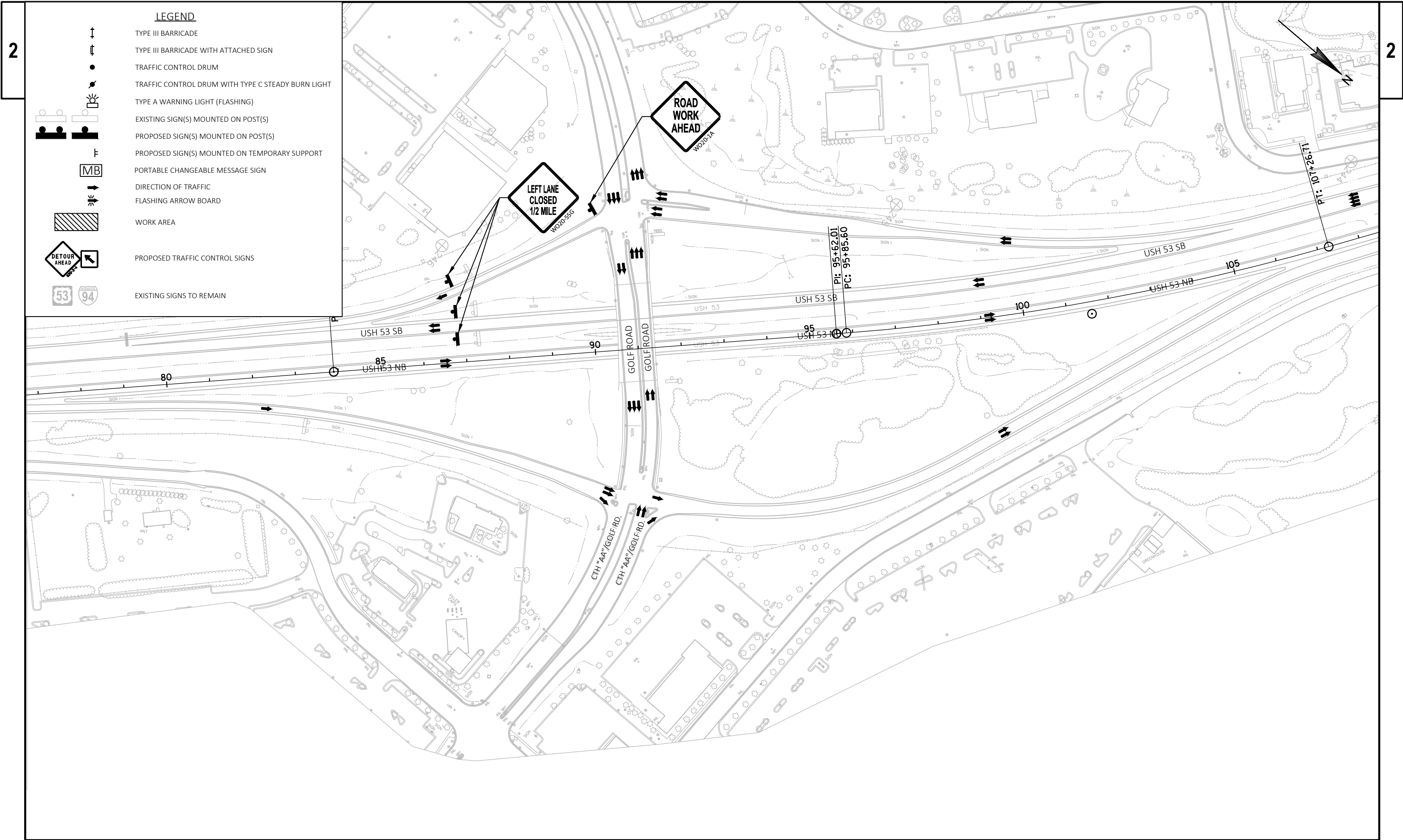
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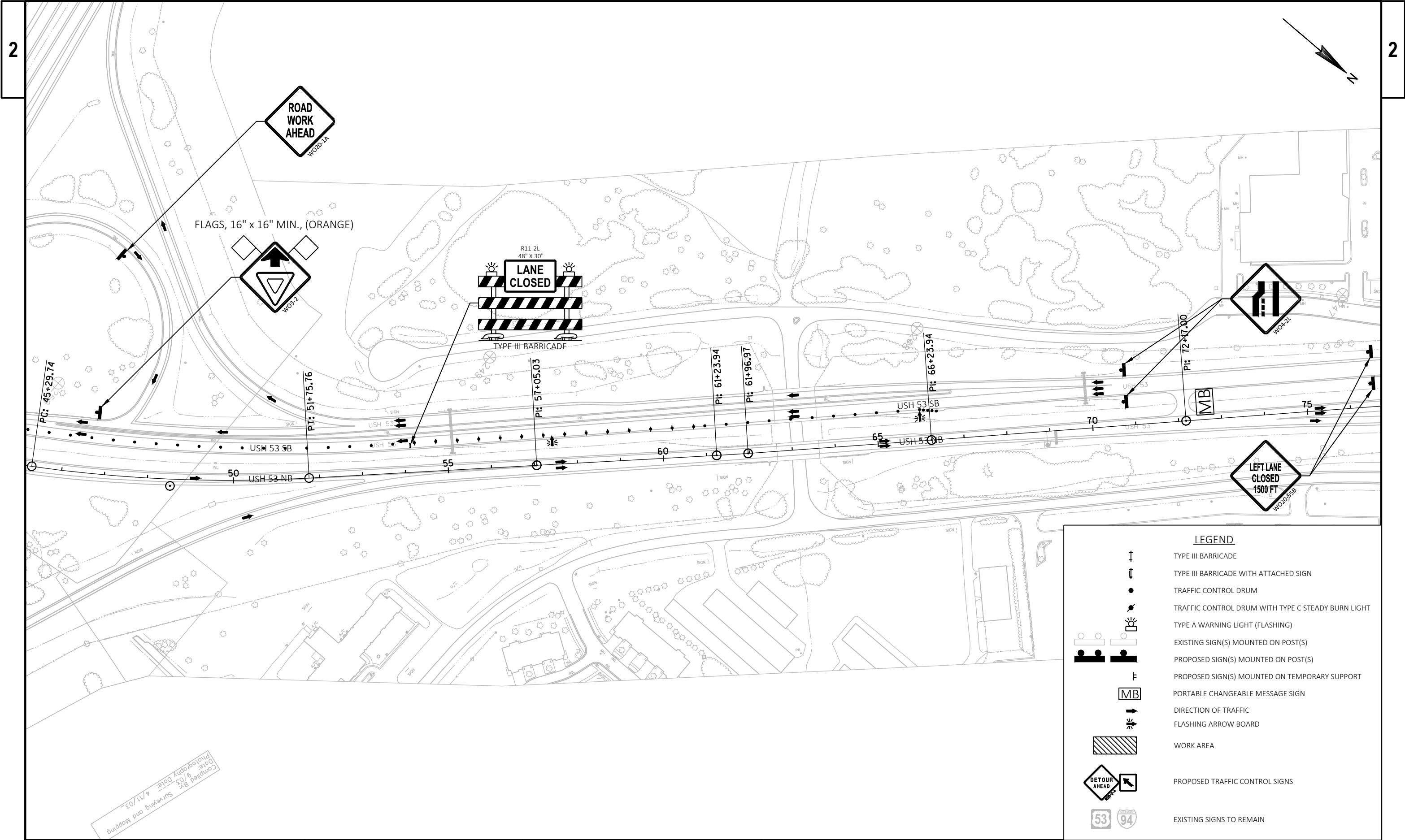




PROJECT NO: 1190-02-64	HWY: USH 53	COUNTY: EAU CLAIRE	TRAFFIC CONTROL - B-18-35 STAGE 2 PLAN	SHEET	E
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PROJECT NO: 1190-02-64

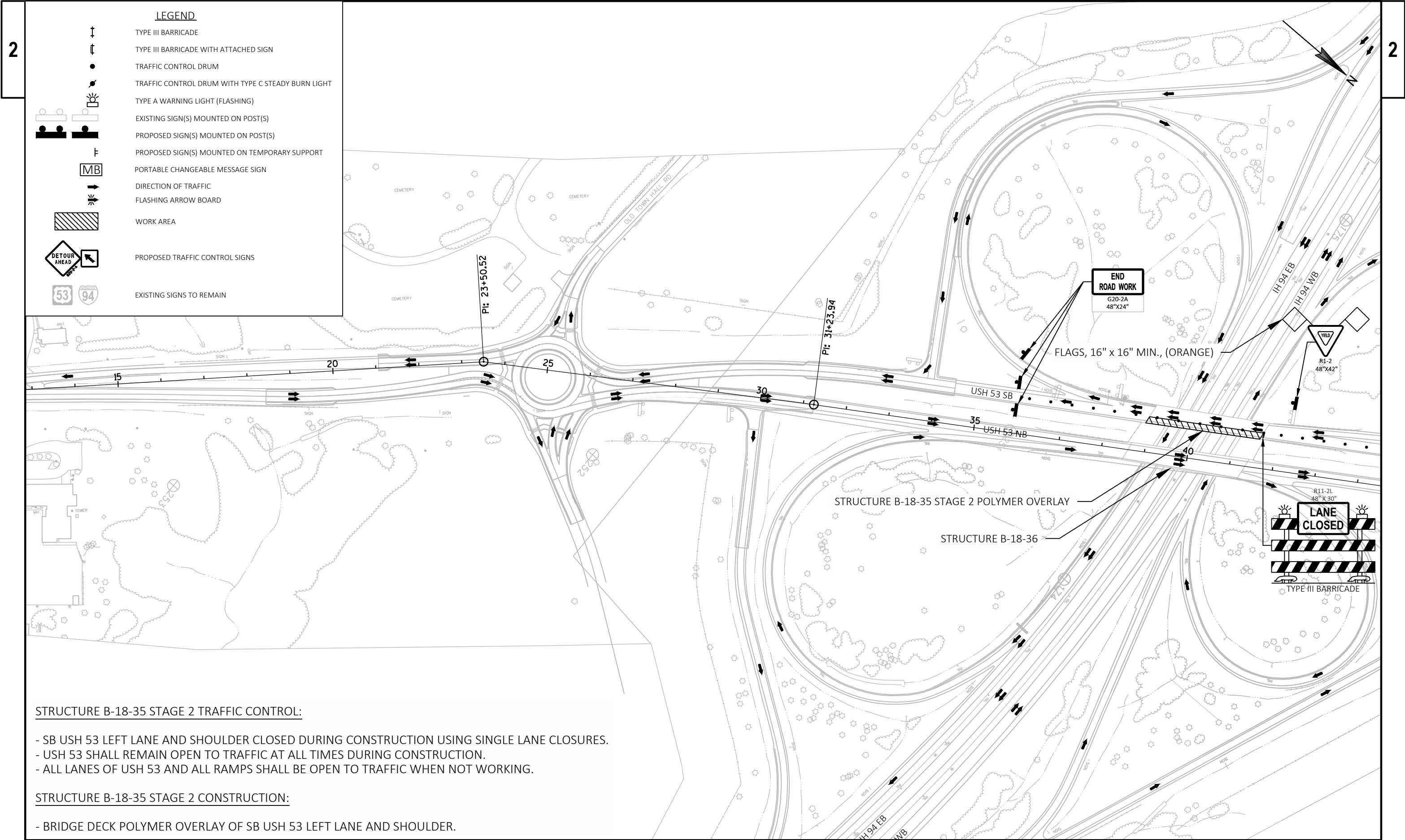
HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - B-18-35 STAGE 2 PLAN

SHEET

E



STRUCTURE B-18-35 STAGE 2 TRAFFIC CONTROL:

- SB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-35 STAGE 2 CONSTRUCTION:

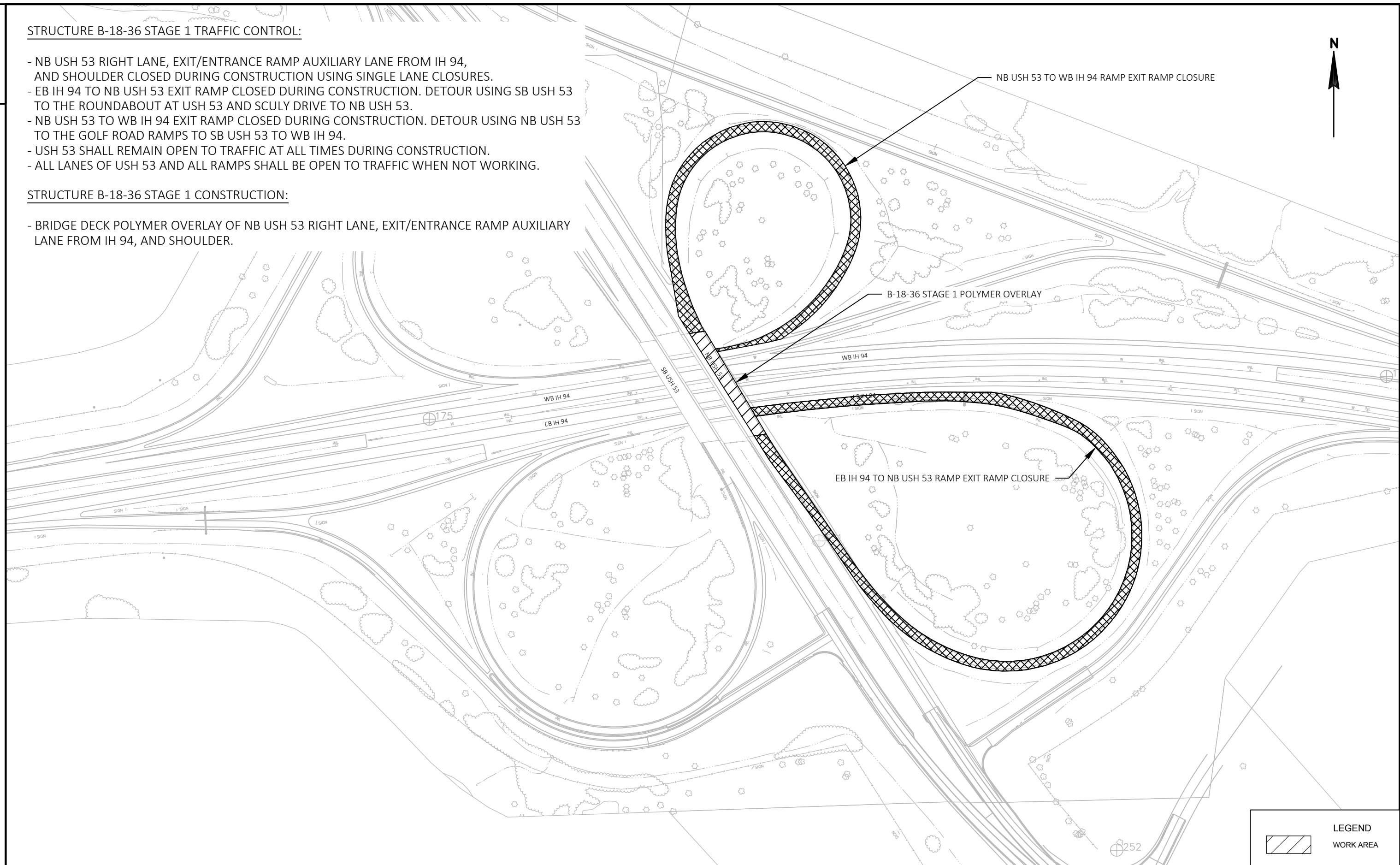
- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 LEFT LANE AND SHOULDER.

STRUCTURE B-18-36 STAGE 1 TRAFFIC CONTROL:

- NB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- EB IH 94 TO NB USH 53 EXIT RAMP CLOSED DURING CONSTRUCTION. DETOUR USING SB USH 53 TO THE ROUNDABOUT AT USH 53 AND SCULY DRIVE TO NB USH 53.
- NB USH 53 TO WB IH 94 EXIT RAMP CLOSED DURING CONSTRUCTION. DETOUR USING NB USH 53 TO THE GOLF ROAD RAMPS TO SB USH 53 TO WB IH 94.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-36 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF NB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER.

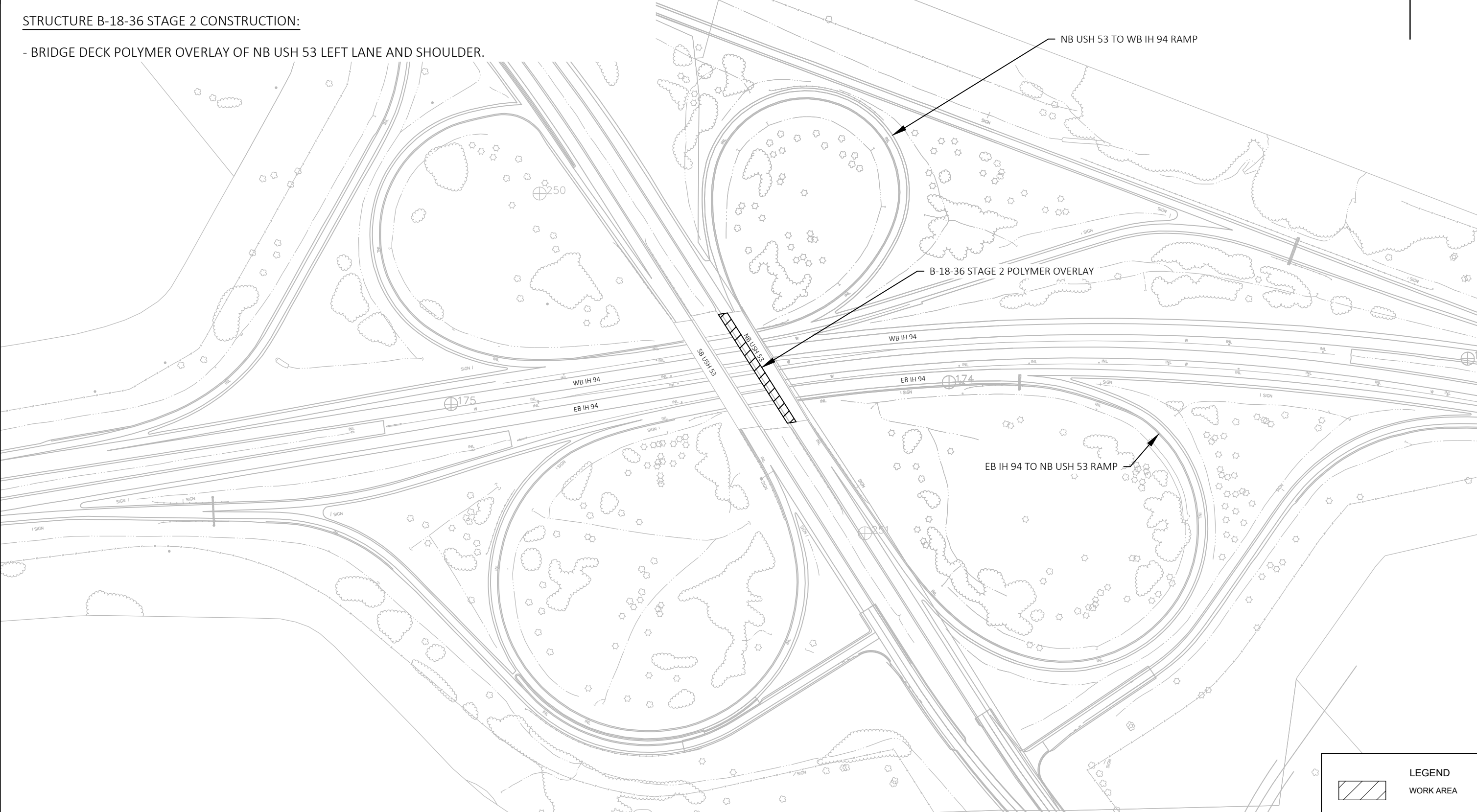


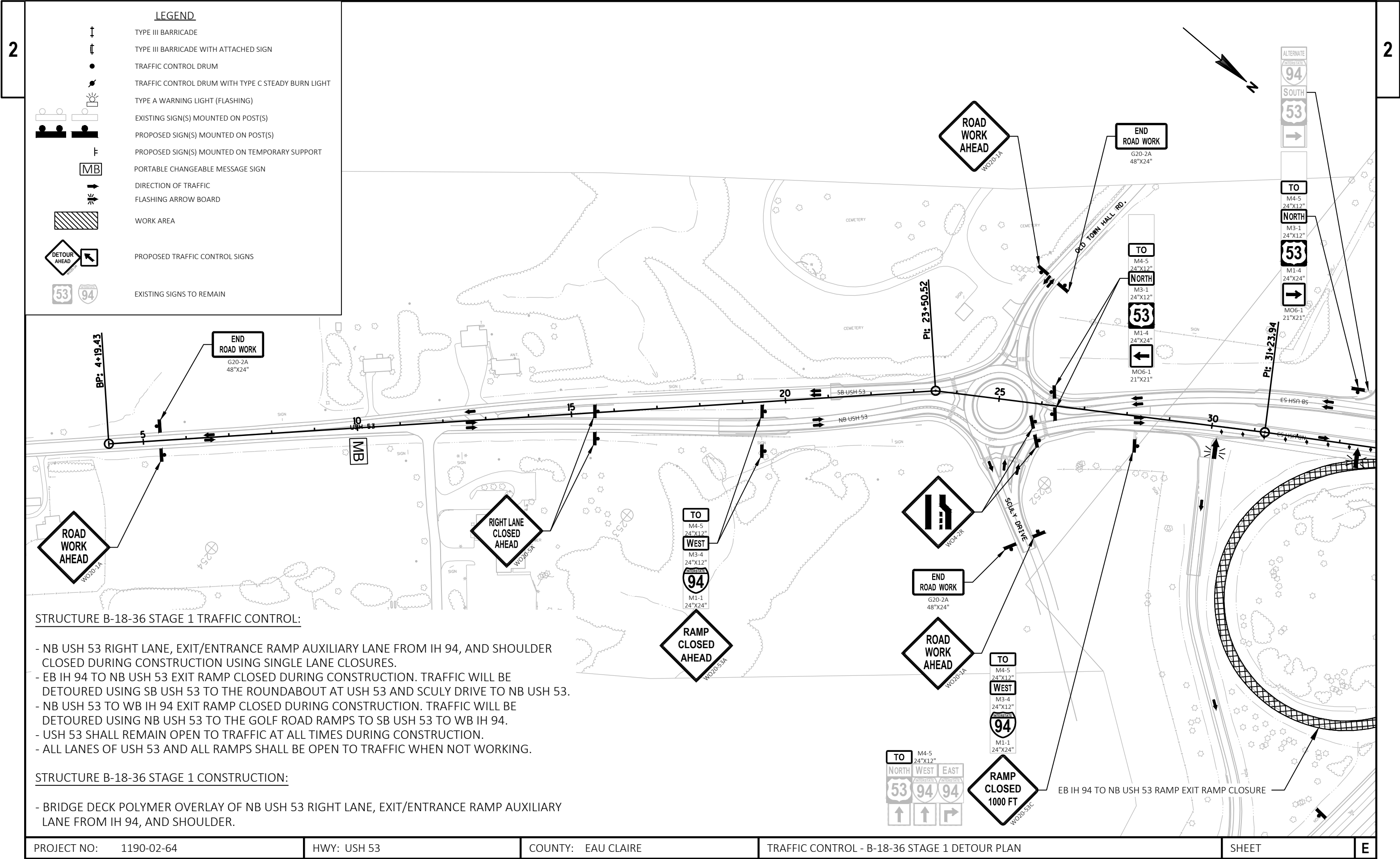
STRUCTURE B-18-36 STAGE 2 TRAFFIC CONTROL:

- NB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-36 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF NB USH 53 LEFT LANE AND SHOULDER.





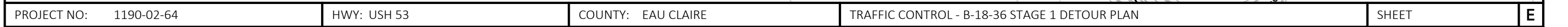
STRUCTURE B-18-36 STAGE 1 TRAFFIC CONTROL:

- NB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- EB IH 94 TO NB USH 53 EXIT RAMP CLOSED DURING CONSTRUCTION. TRAFFIC WILL BE DETOURED USING SB USH 53 TO THE ROUNDABOUT AT USH 53 AND SCULY DRIVE TO NB USH 53.
- NB USH 53 TO WB IH 94 EXIT RAMP CLOSED DURING CONSTRUCTION. TRAFFIC WILL BE DETOURED USING NB USH 53 TO THE GOLF ROAD RAMPS TO SB USH 53 TO WB IH 94.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-36 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF NB USH 53 RIGHT LANE, EXIT/ENTRANCE RAMP AUXILIARY LANE FROM IH 94, AND SHOULDER.







TYPE III BARRICADE

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

TYPE A WARNING LIGHT (FLASHING)

EXISTING SIGN(S) MOUNTED ON POST(S)

PROPOSED SIGN(S) MOUNTED ON POST(S)

PROPOSED SIGN(S) MOUNTED ON TEMPORARY SUPPORT

PORTABLE CHANGEABLE MESSAGE SIGN

DIRECTION OF TRAFFIC

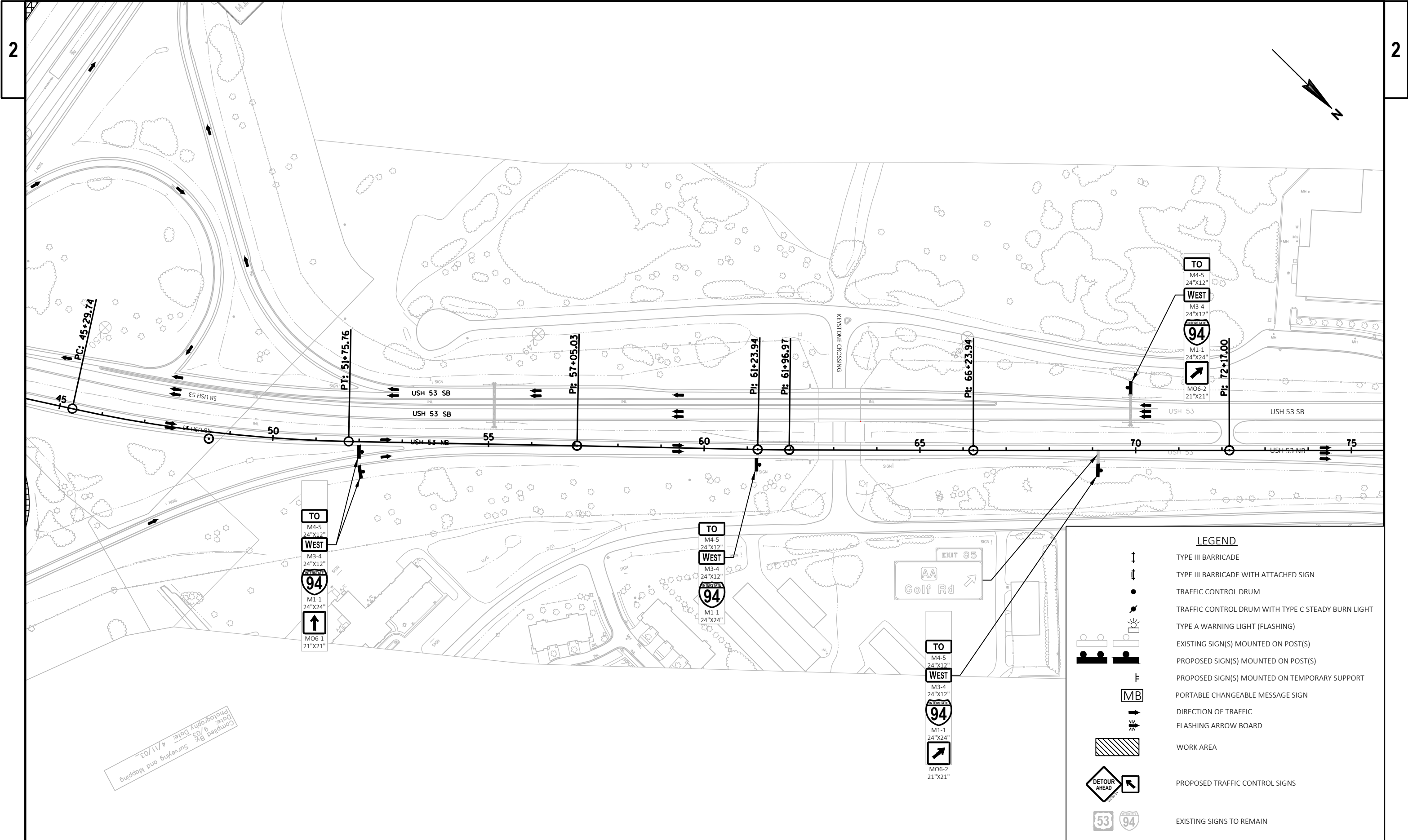
FLASHING ARROW BOARD

WORK AREA

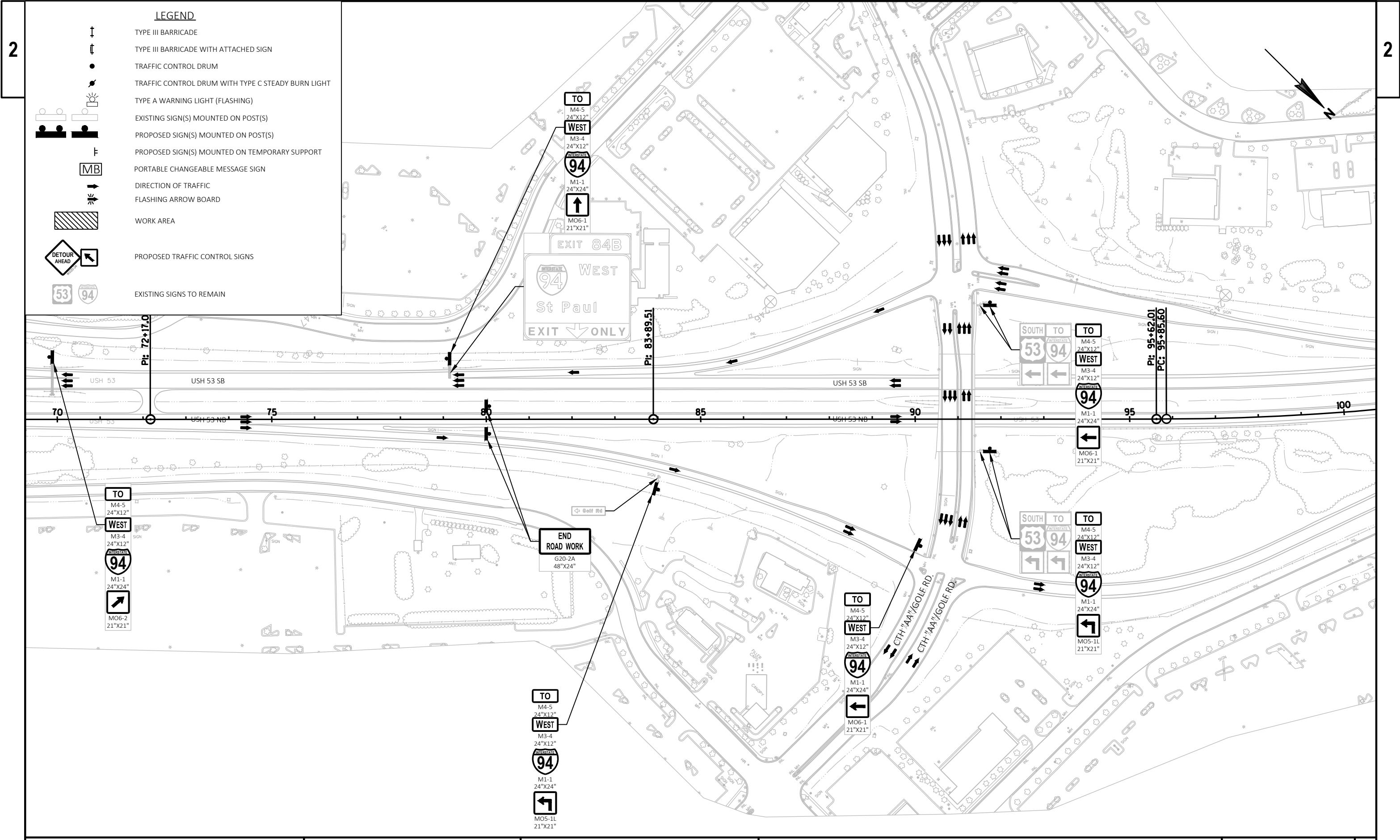
PROPOSED TRAFFIC CONTROL SIGNS

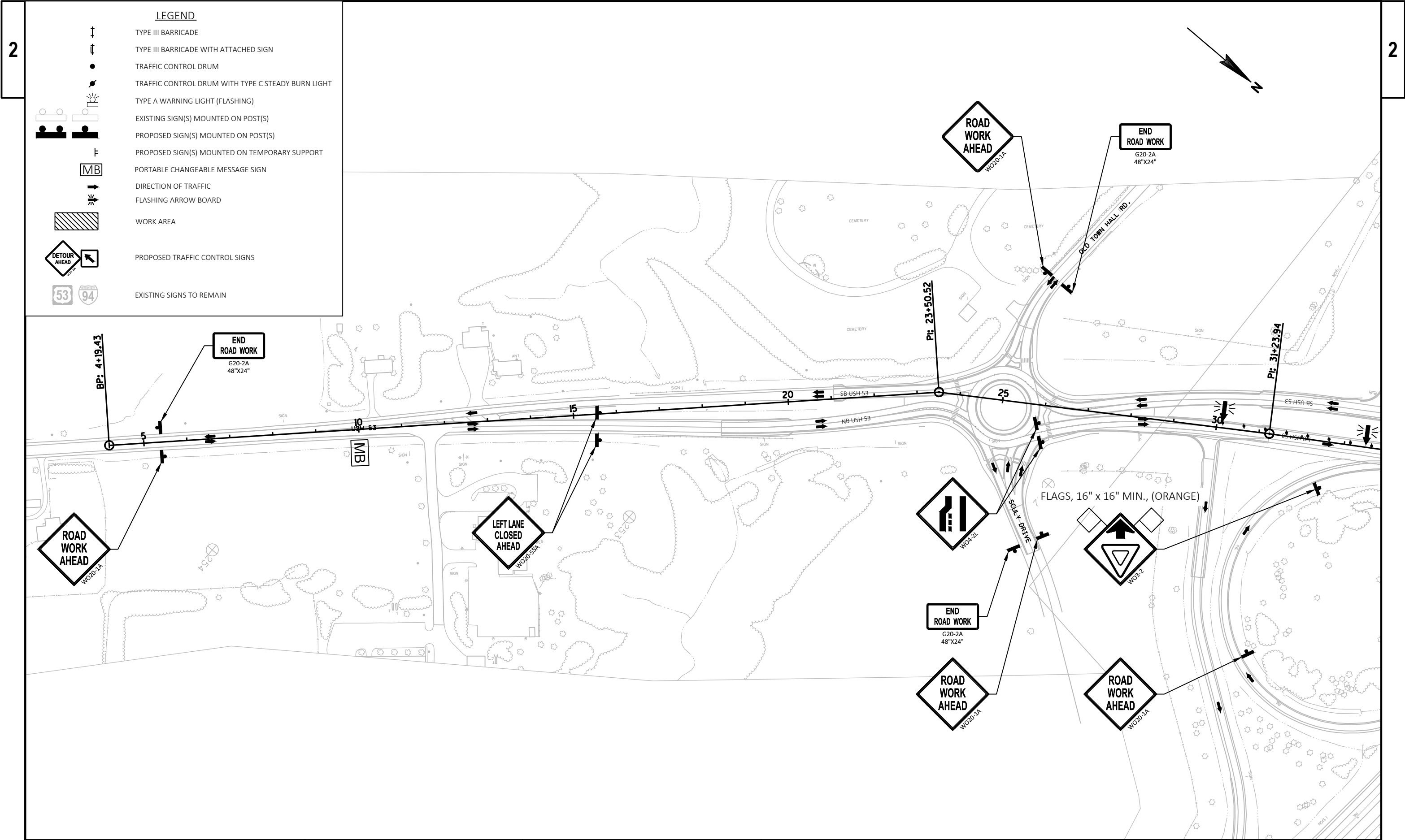
EXISTING SIGNS TO REMAIN

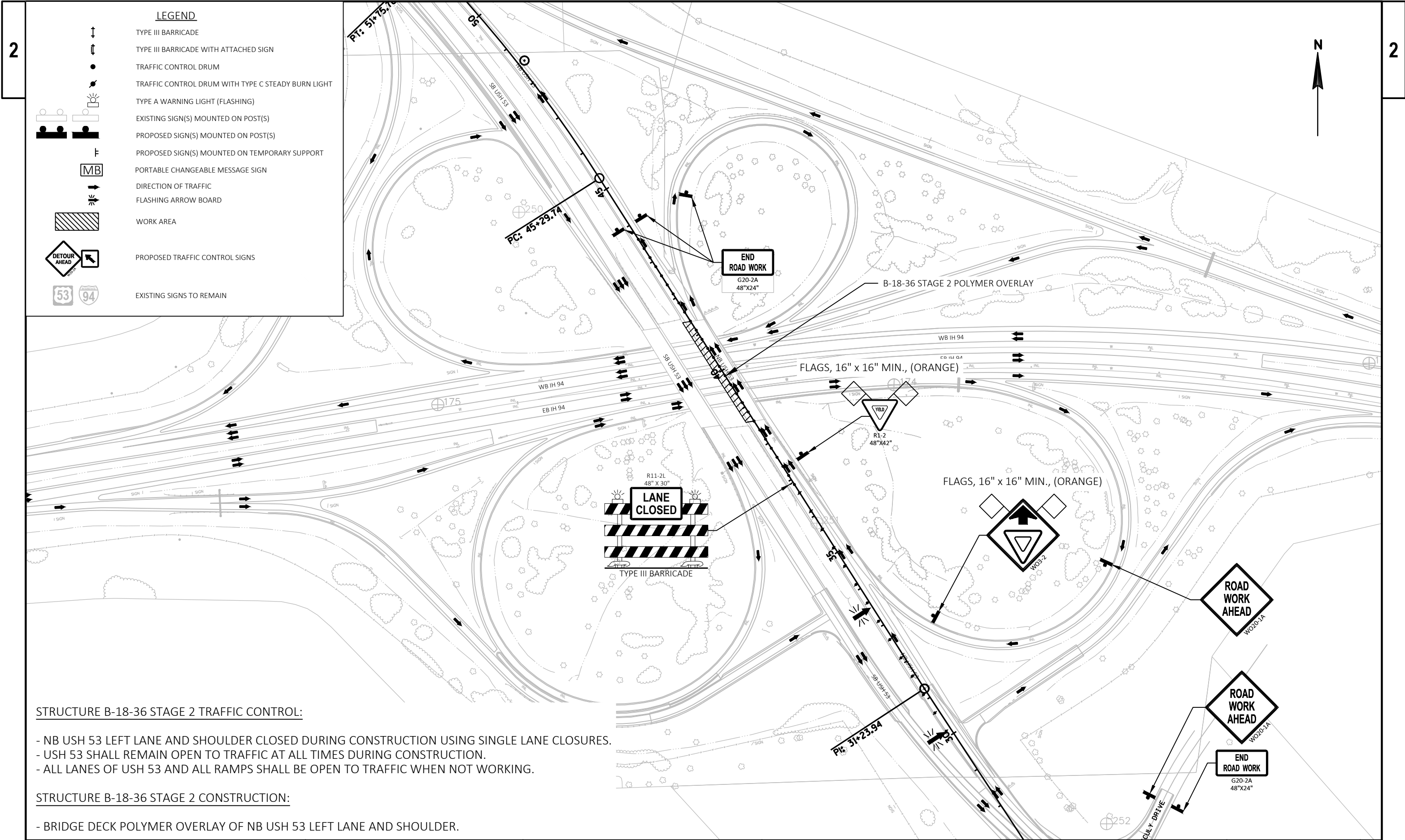




Compiled By: [Name]  
Date: 9/03  
Photography Date: 4/11/03  
Surveying and Mapping







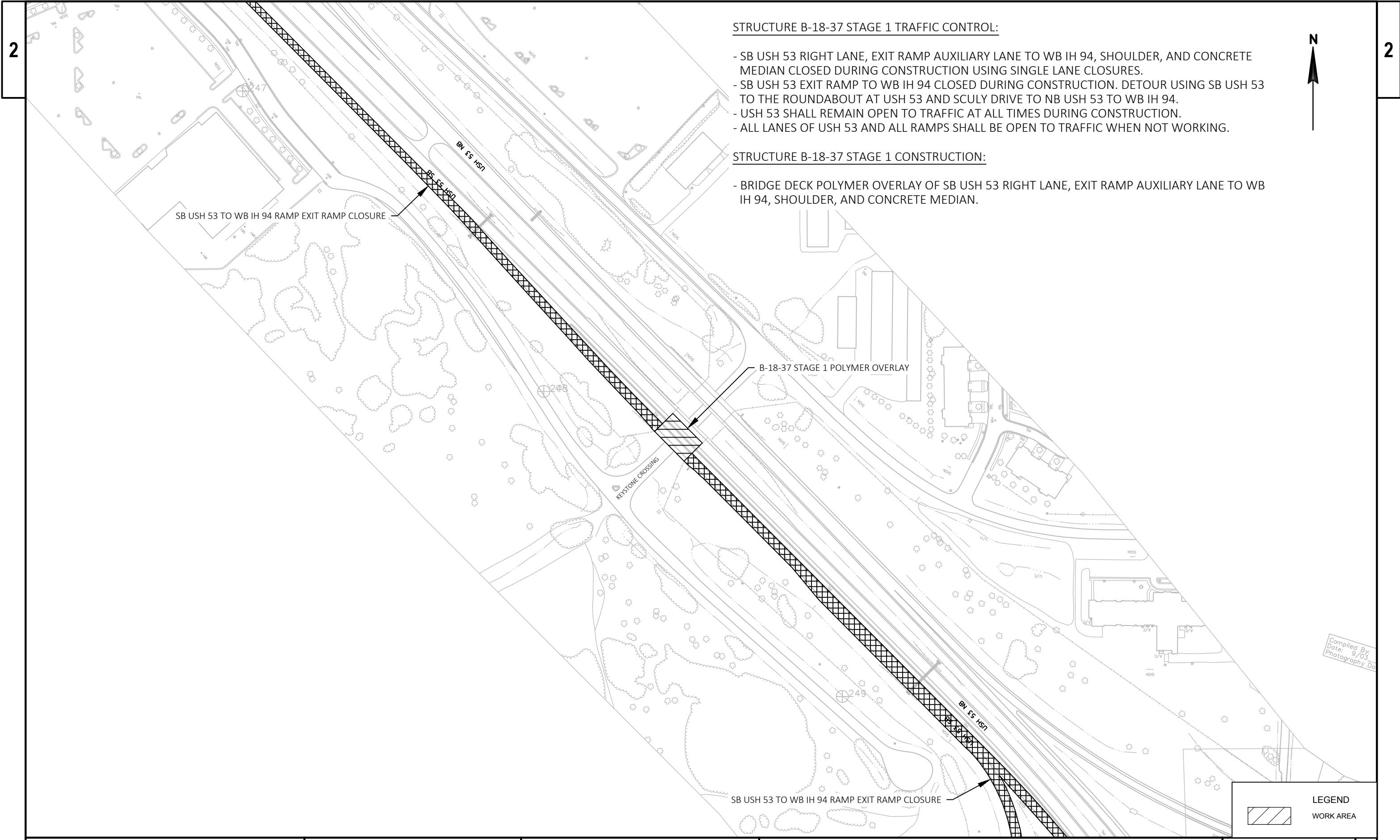
STRUCTURE B-18-36 STAGE 2 TRAFFIC CONTROL:

- NB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-36 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF NB USH 53 LEFT LANE AND SHOULDER.





STRUCTURE B-18-37 STAGE 1 TRAFFIC CONTROL:

- SB USH 53 RIGHT LANE, EXIT RAMP AUXILIARY LANE TO WB IH 94, SHOULDER, AND CONCRETE MEDIAN CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- SB USH 53 EXIT RAMP TO WB IH 94 CLOSED DURING CONSTRUCTION. DETOUR USING SB USH 53 TO THE ROUNDABOUT AT USH 53 AND SCULY DRIVE TO NB USH 53 TO WB IH 94.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPs SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-37 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 RIGHT LANE, EXIT RAMP AUXILIARY LANE TO WB IH 94, SHOULDER, AND CONCRETE MEDIAN.



STRUCTURE B-18-37 STAGE 2 TRAFFIC CONTROL:

- SB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-37 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 LANE AND SHOULDER.

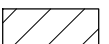


SB USH 53 TO WB IH 94 RAMP

B-18-37 STAGE 2 POLYMER OVERLAY

KEYSTONE CROSSING

SB USH 53 TO WB IH 94 RAMP



LEGEND  
WORK AREA

PROJECT NO: 1190-02-64

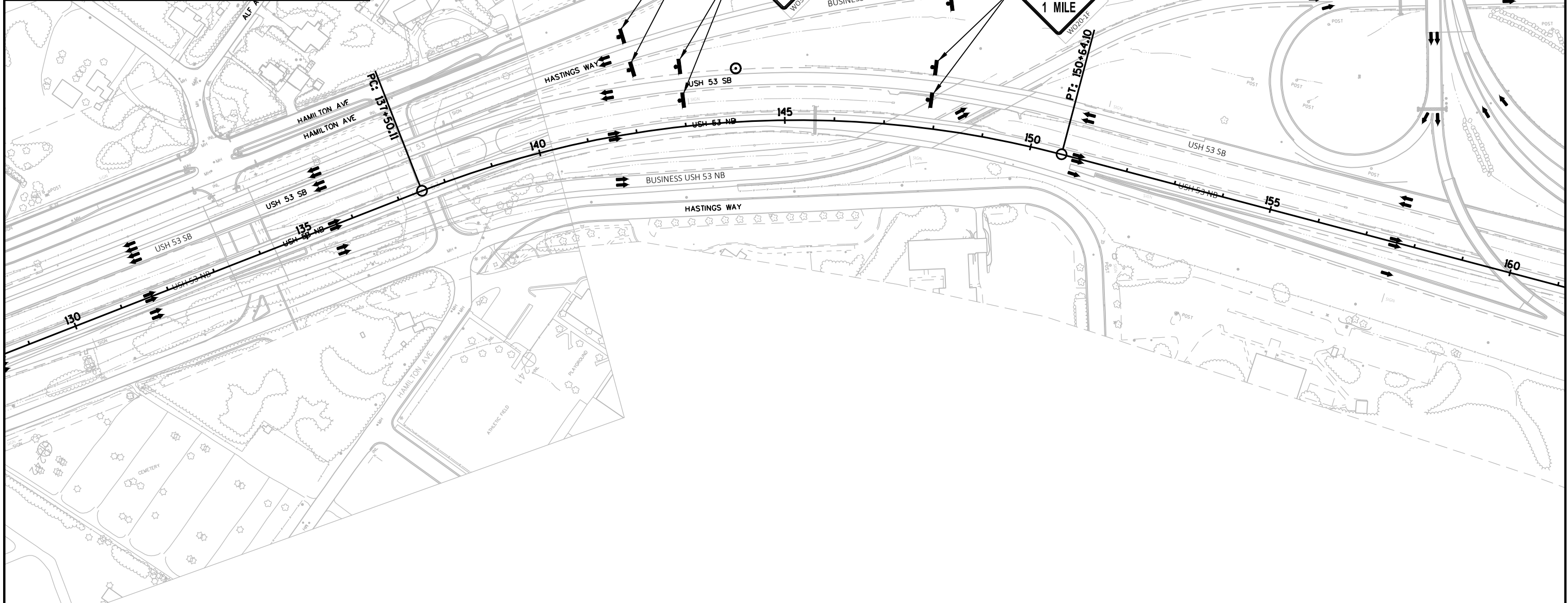
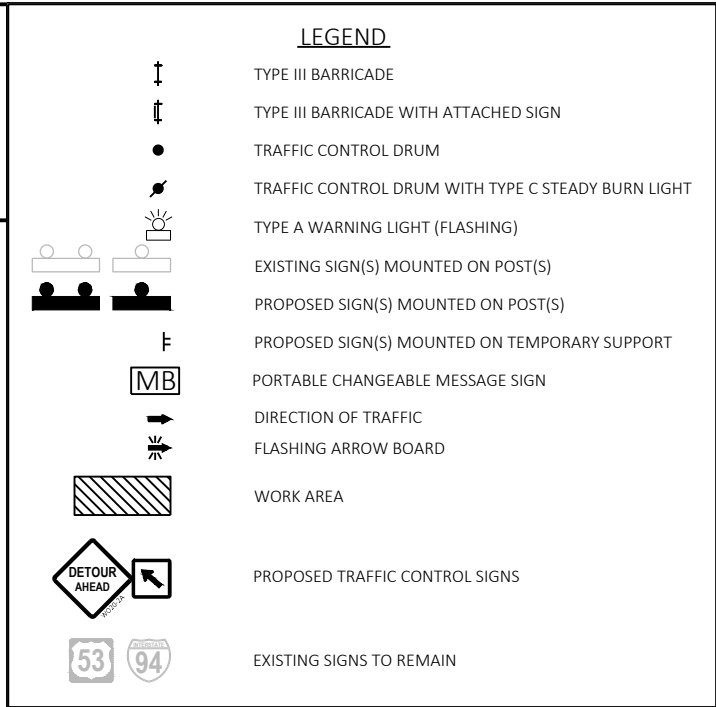
HWY: USH 53

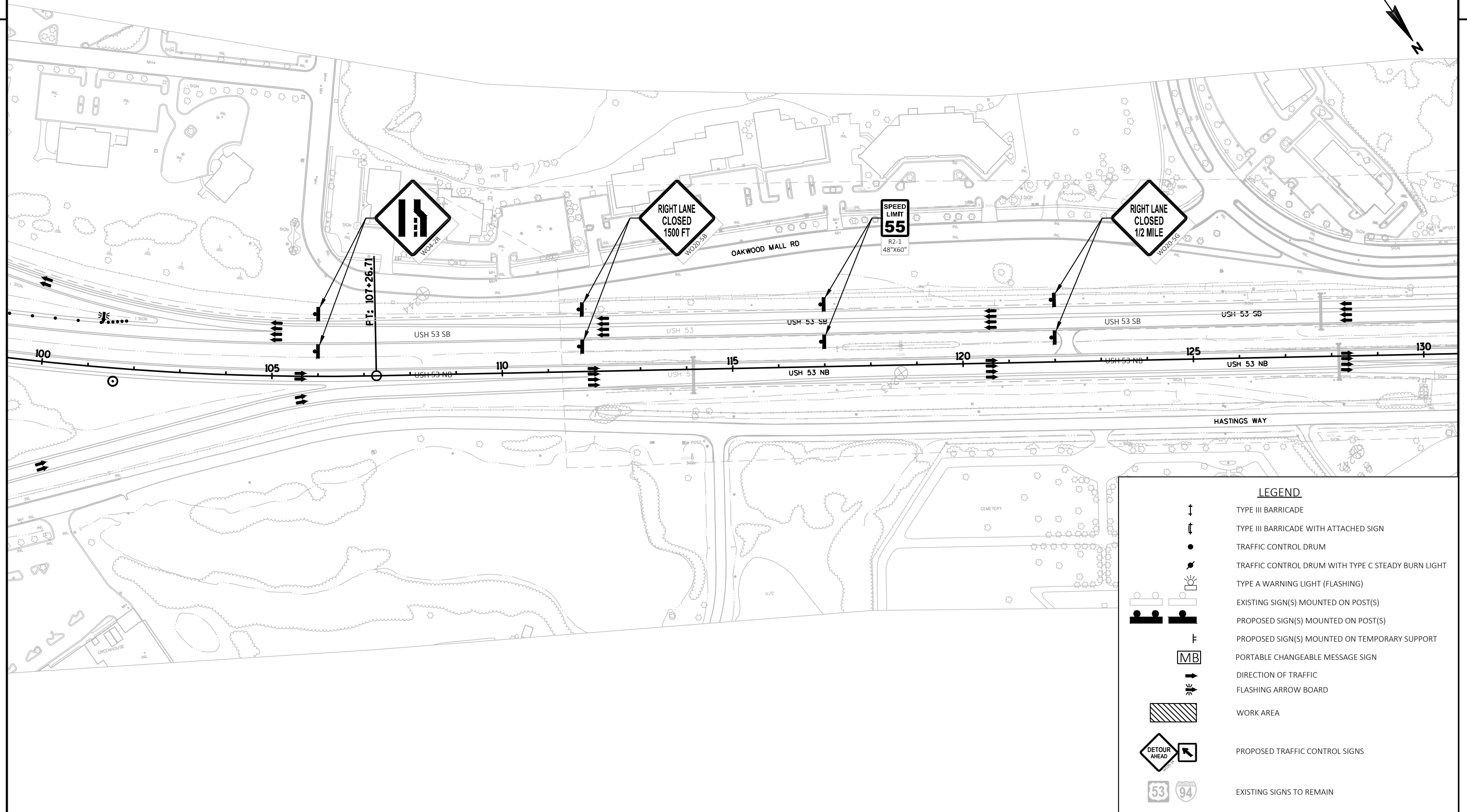
COUNTY: EAU CLAIRE

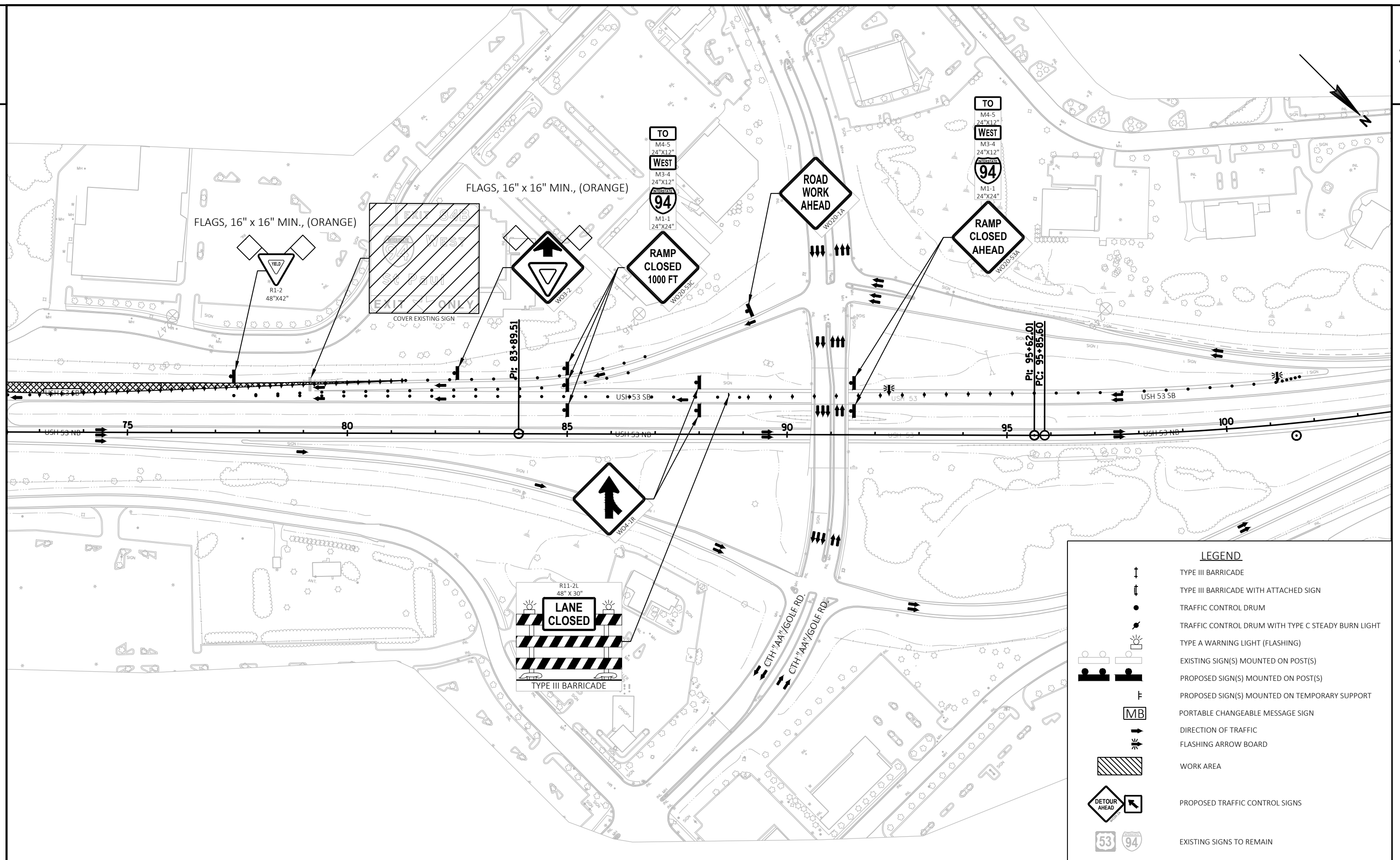
PLAN: TRAFFIC CONTROL - STAGE 2 OVERVIEW - B-18-37

SHEET

E







PROJECT NO: 1190-02-64

HWY: USH 53

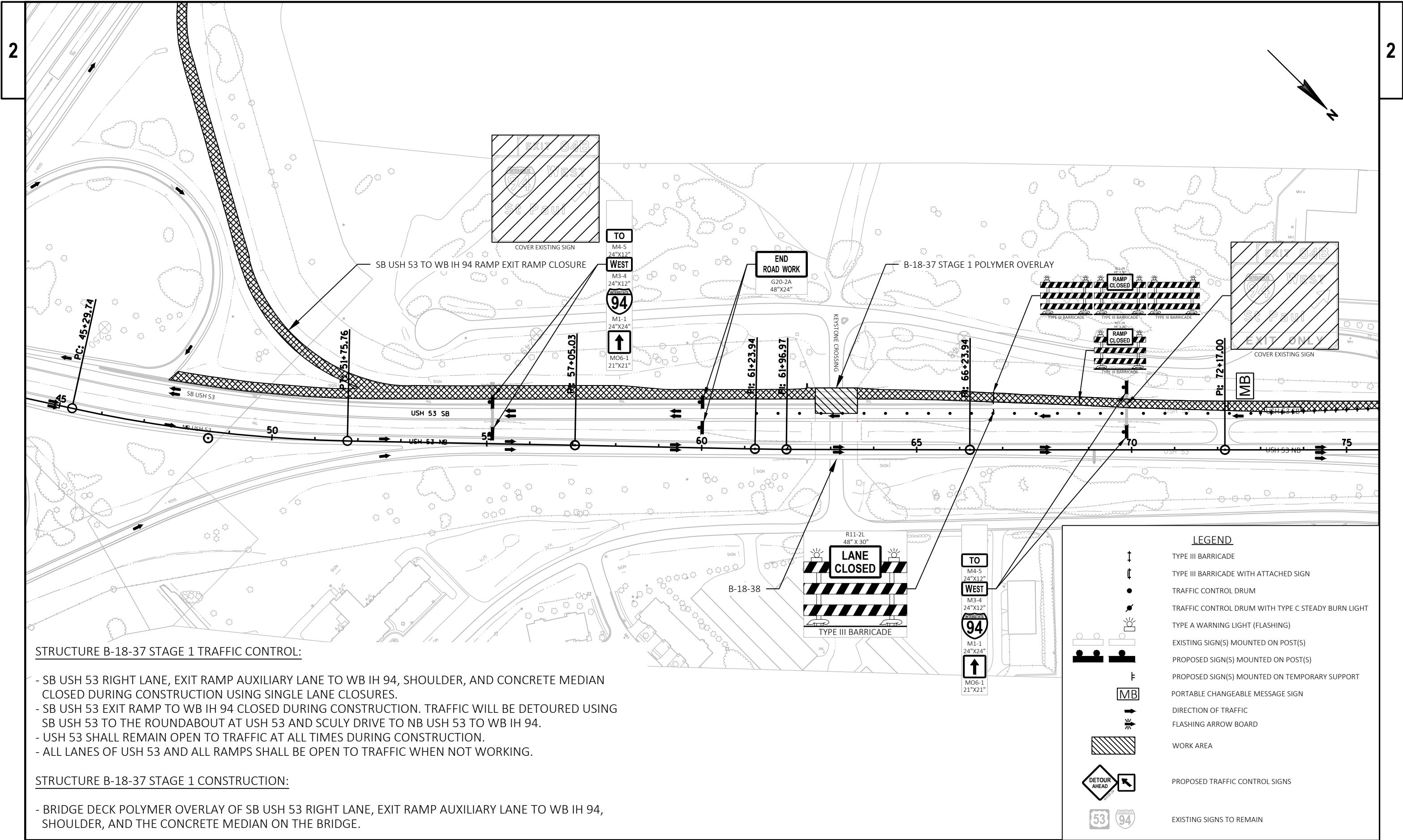
COUNTY: EAU CLAIRE

TRAFFIC CONTROL - B-18-37 STAGE 1 DETOUR PLAN

SHEET

E



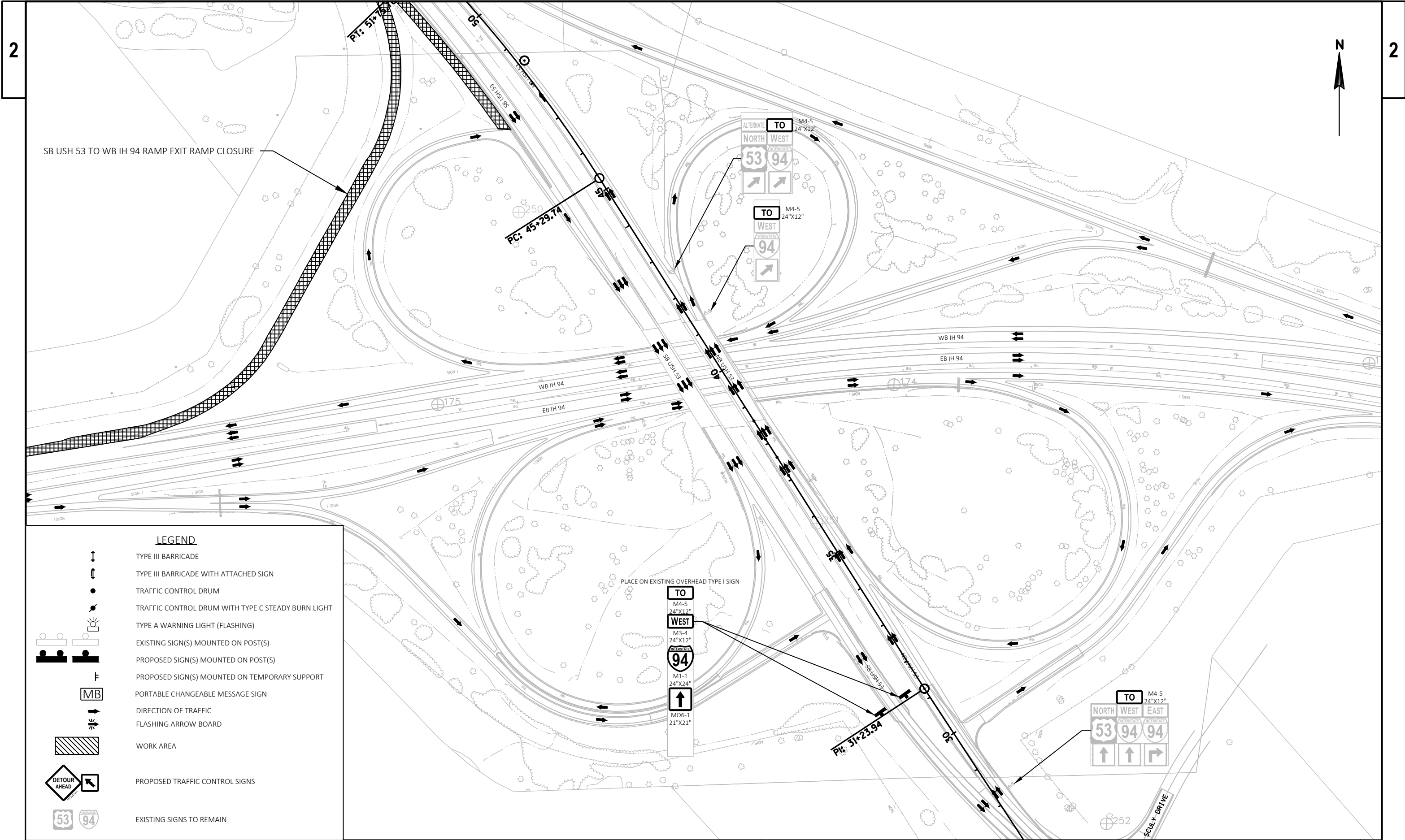


STRUCTURE B-18-37 STAGE 1 TRAFFIC CONTROL:

- SB USH 53 RIGHT LANE, EXIT RAMP AUXILIARY LANE TO WB IH 94, SHOULDER, AND CONCRETE MEDIAN CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- SB USH 53 EXIT RAMP TO WB IH 94 CLOSED DURING CONSTRUCTION. TRAFFIC WILL BE DETOURED USING SB USH 53 TO THE ROUNDABOUT AT USH 53 AND SCULY DRIVE TO NB USH 53 TO WB IH 94.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-37 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 RIGHT LANE, EXIT RAMP AUXILIARY LANE TO WB IH 94, SHOULDER, AND THE CONCRETE MEDIAN ON THE BRIDGE.





TYPE III BARRICADE

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

TYPE A WARNING LIGHT (FLASHING)

EXISTING SIGN(S) MOUNTED ON POST(S)

PROPOSED SIGN(S) MOUNTED ON POST(S)

PROPOSED SIGN(S) MOUNTED ON TEMPORARY SUPPORT

PORTABLE CHANGEABLE MESSAGE SIGN

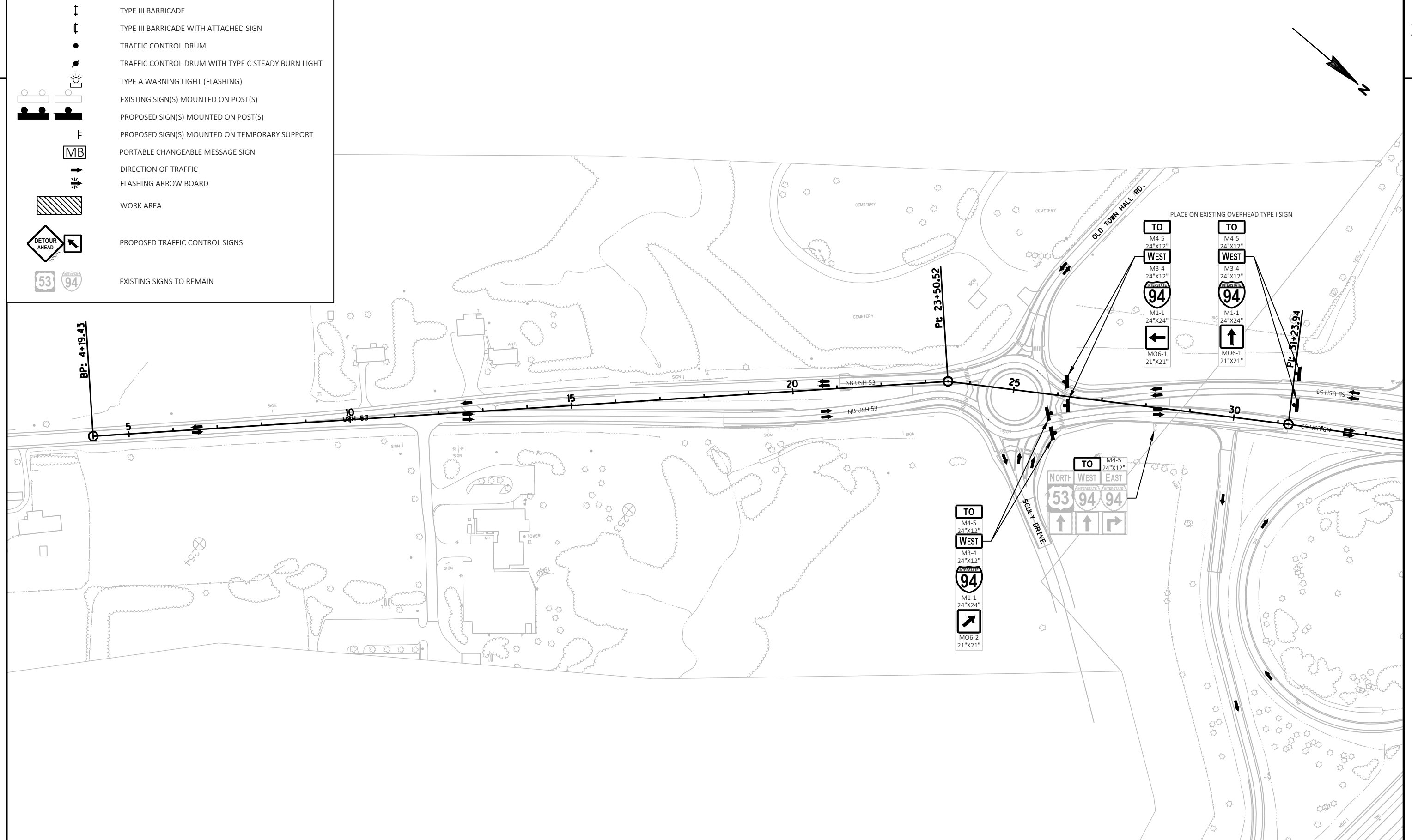
DIRECTION OF TRAFFIC

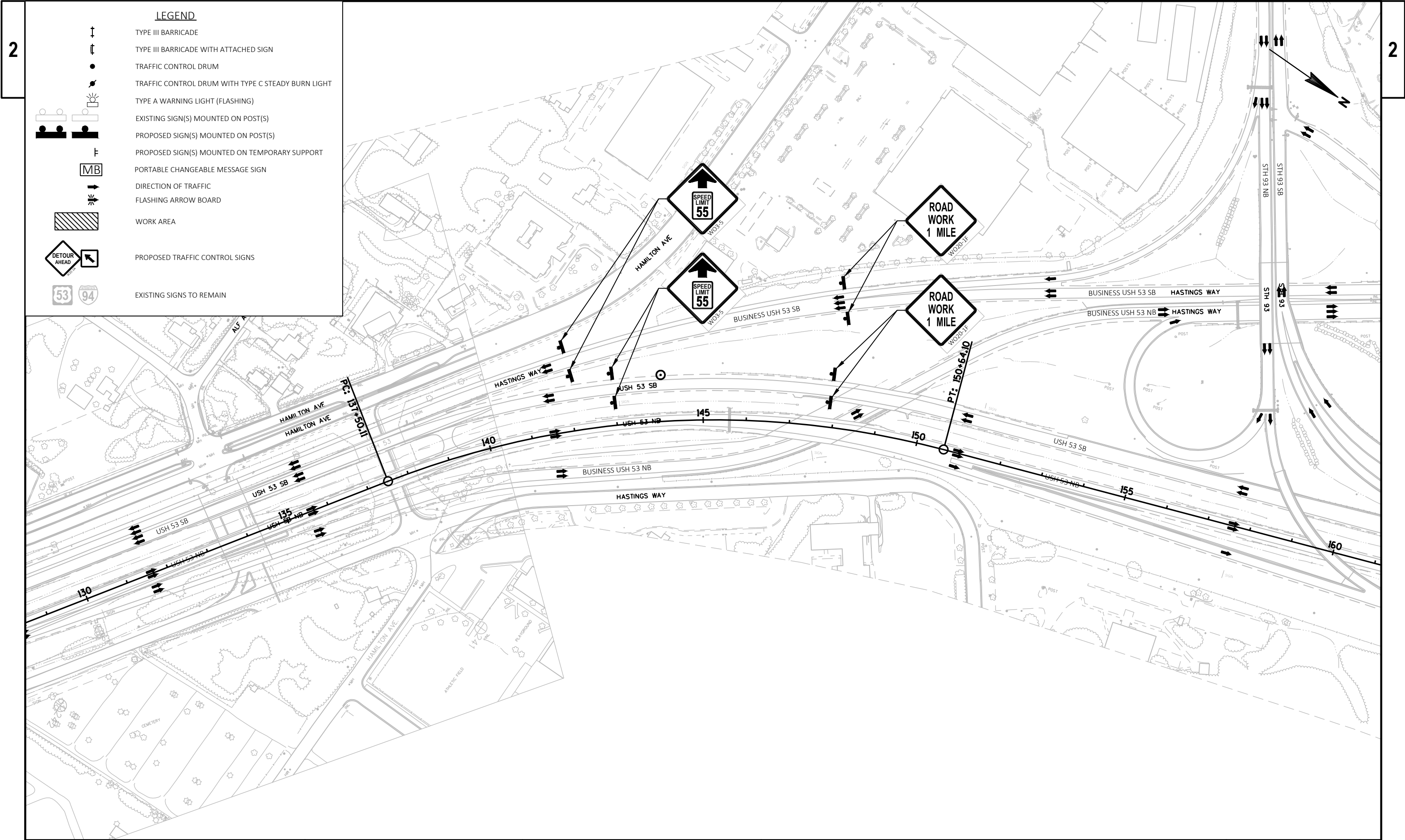
FLASHING ARROW BOARD

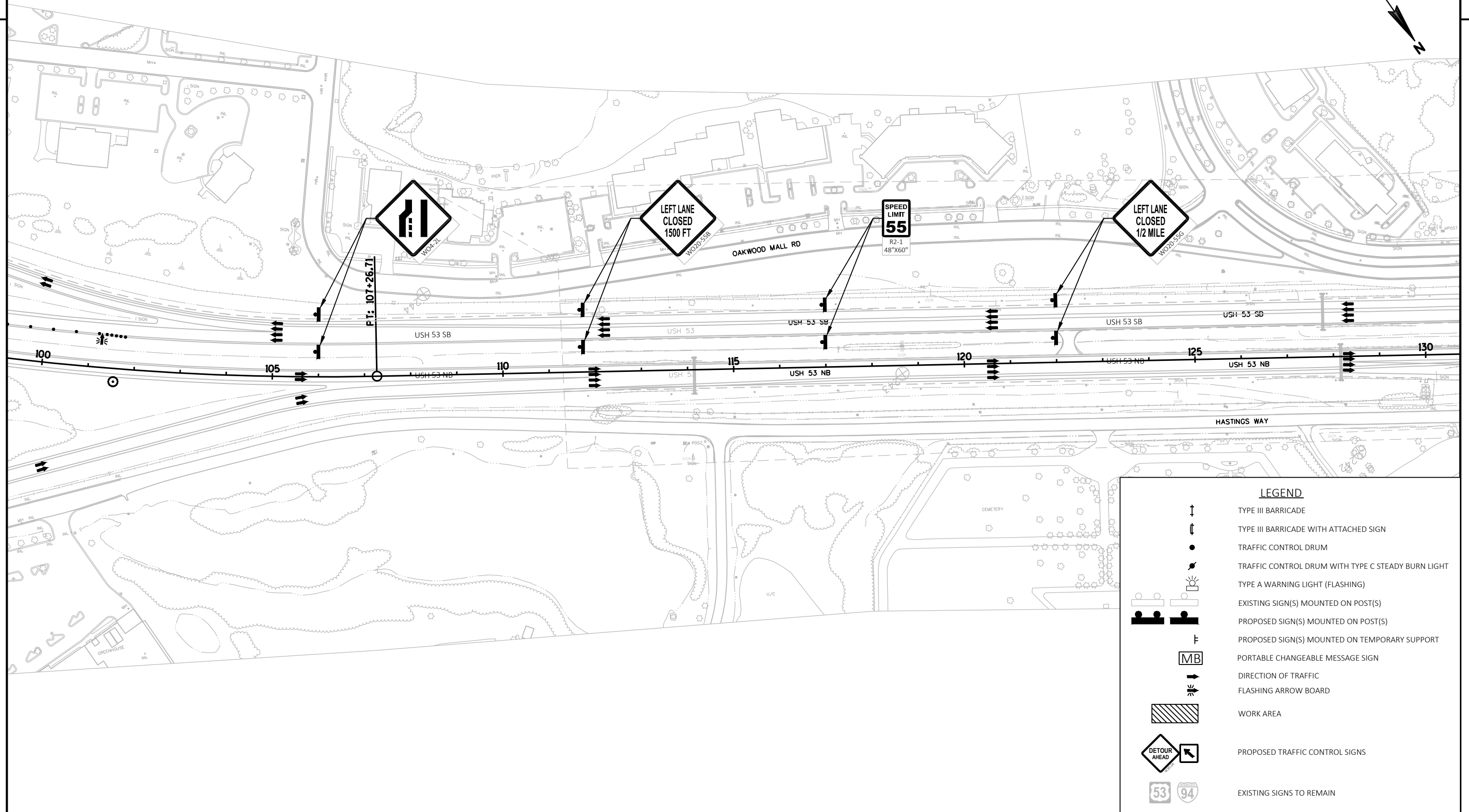
WORK AREA

PROPOSED TRAFFIC CONTROL SIGNS

EXISTING SIGNS TO REMAIN







PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - B-18-37 STAGE 2 PLAN

SHEET

E

FILE NAME : N:\PDS\C3D\11900264\SHEETSP\B-18-37\027000-DT\_STAGE 2.DWG  
LAYOUT NAME - B-18-37 STAGE 2-2

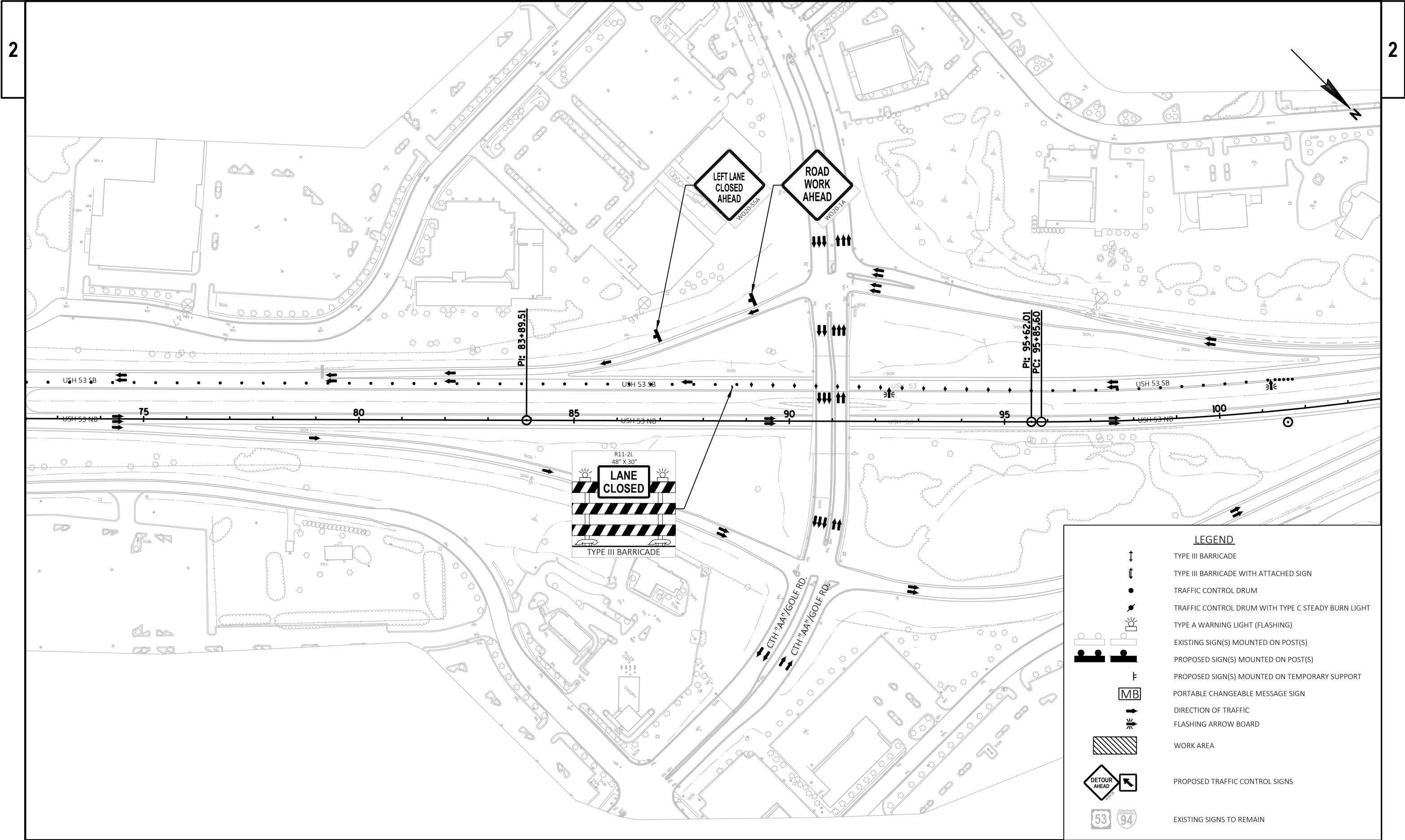
PLOT DATE : 11/7/2018 3:04 PM

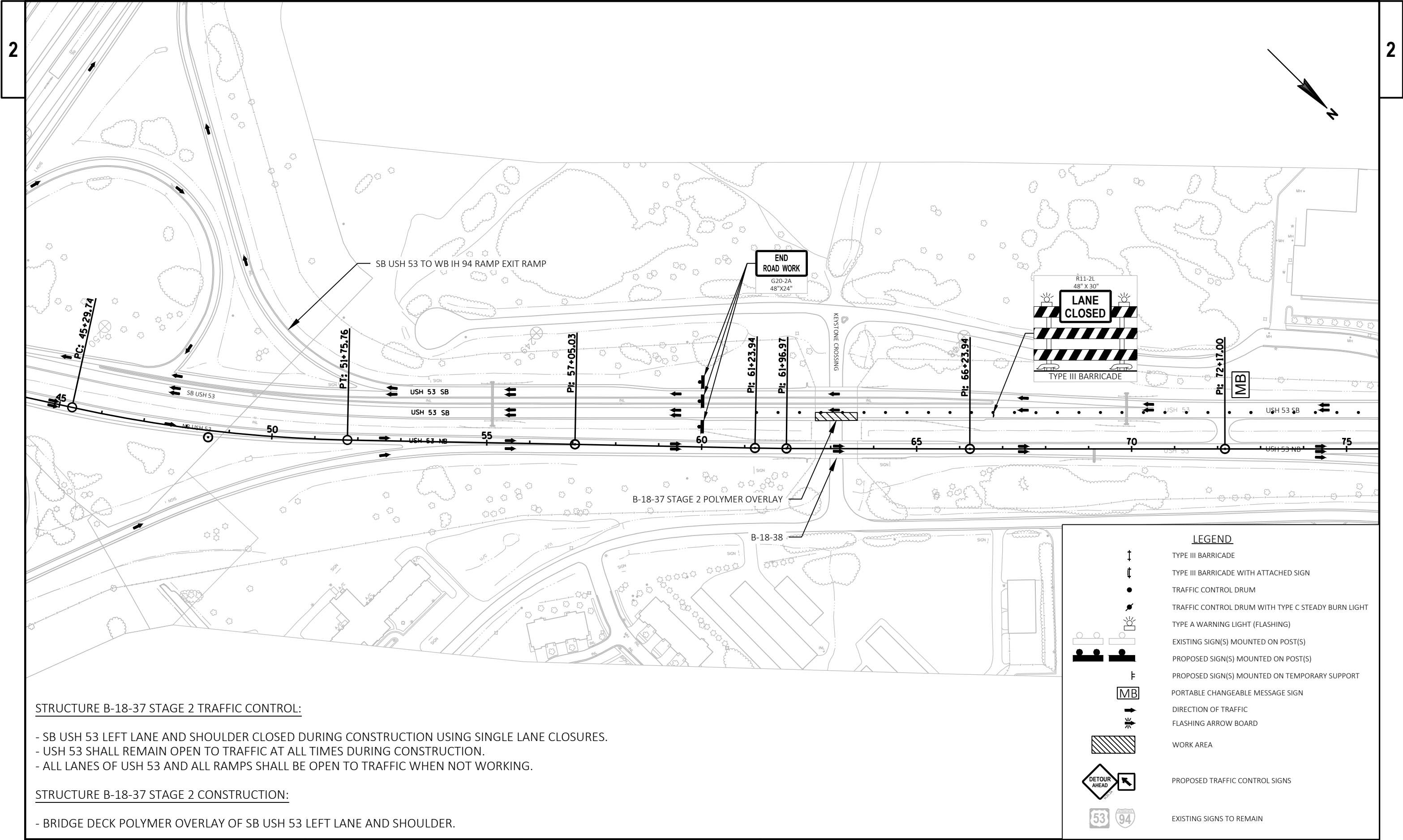
PLOT BY : HETRICK, ADAM M

PLOT NAME :

PLOT SCALE : 1 IN:200 FT

WISDOT/CADDs SHEET 42





STRUCTURE B-18-37 STAGE 2 TRAFFIC CONTROL:

- SB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-37 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF SB USH 53 LEFT LANE AND SHOULDER.

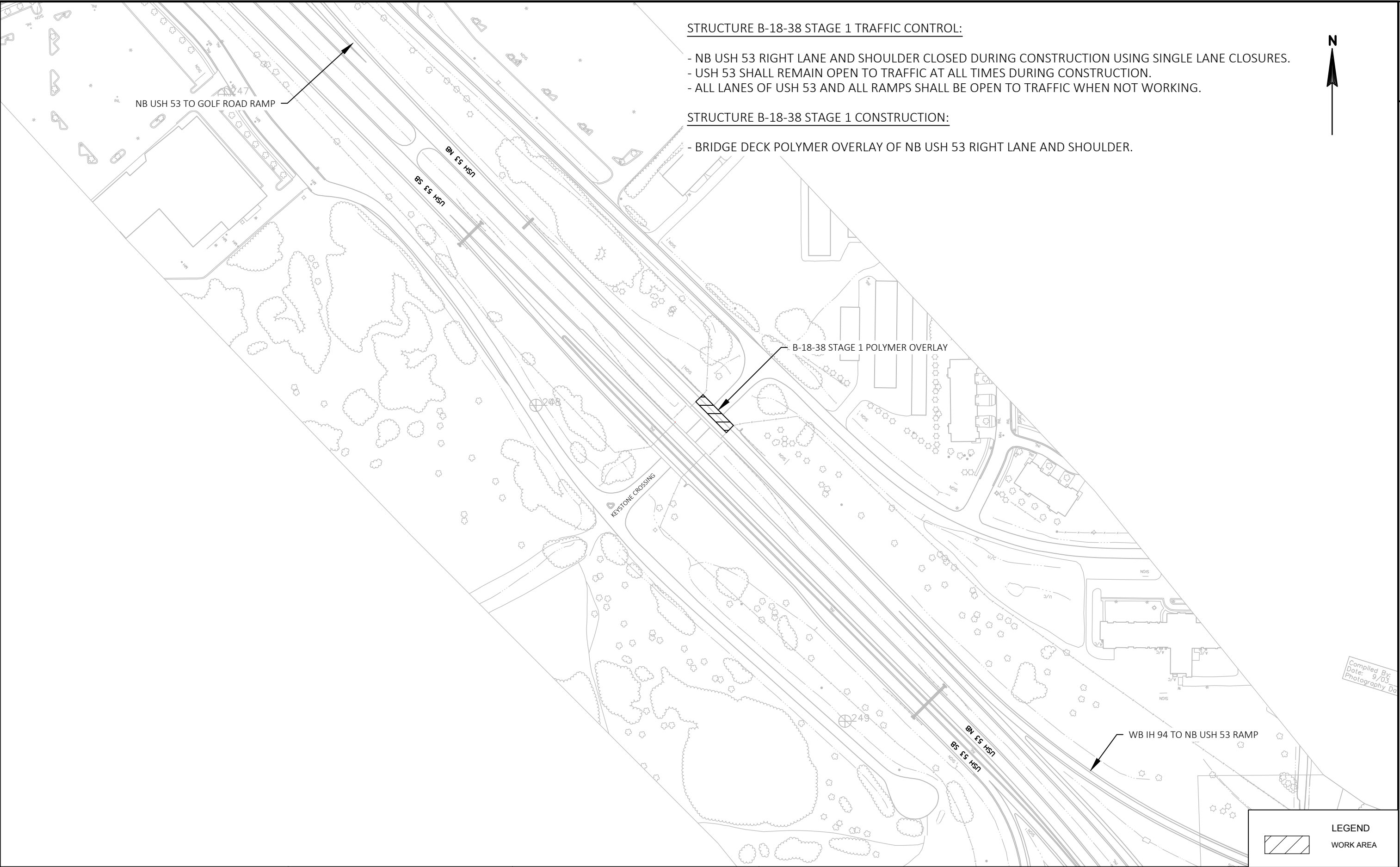


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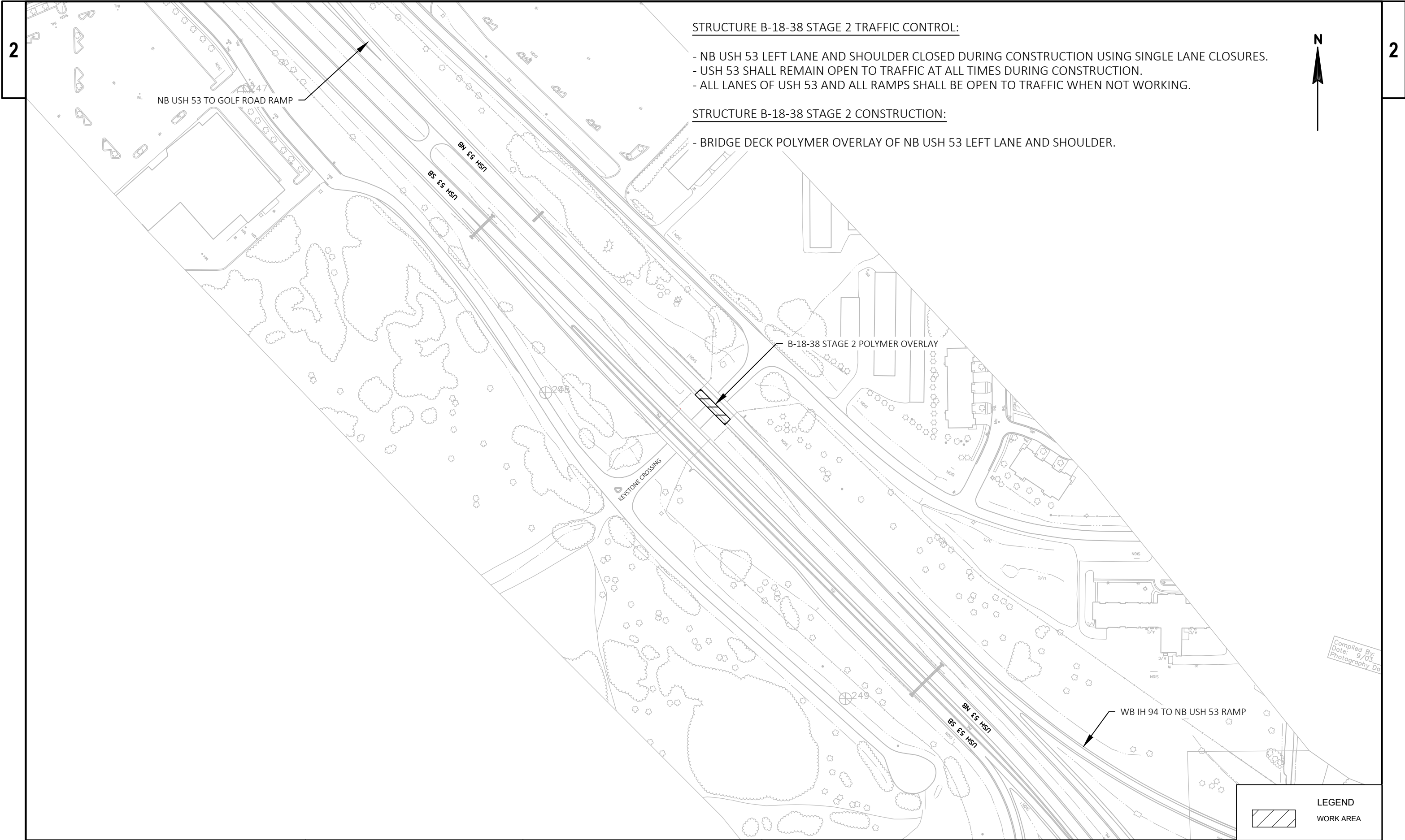
- NB USH 53 RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-38 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF NB USH 53 RIGHT LANE AND SHOULDER.







STRUCTURE B-18-38 STAGE 2 TRAFFIC CONTROL:

- NB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-38 STAGE 2 CONSTRUCTION:

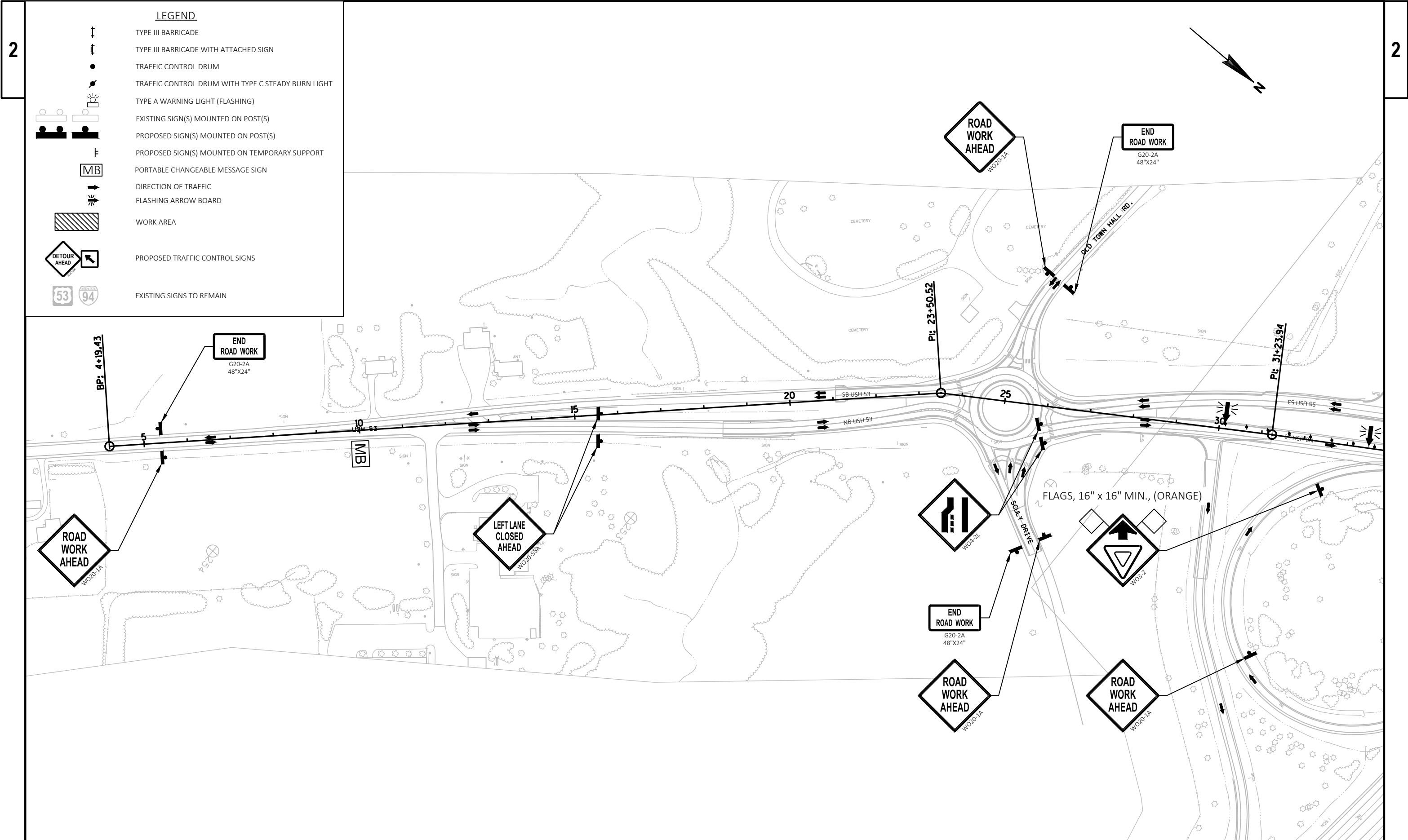
- BRIDGE DECK POLYMER OVERLAY OF NB USH 53 LEFT LANE AND SHOULDER.

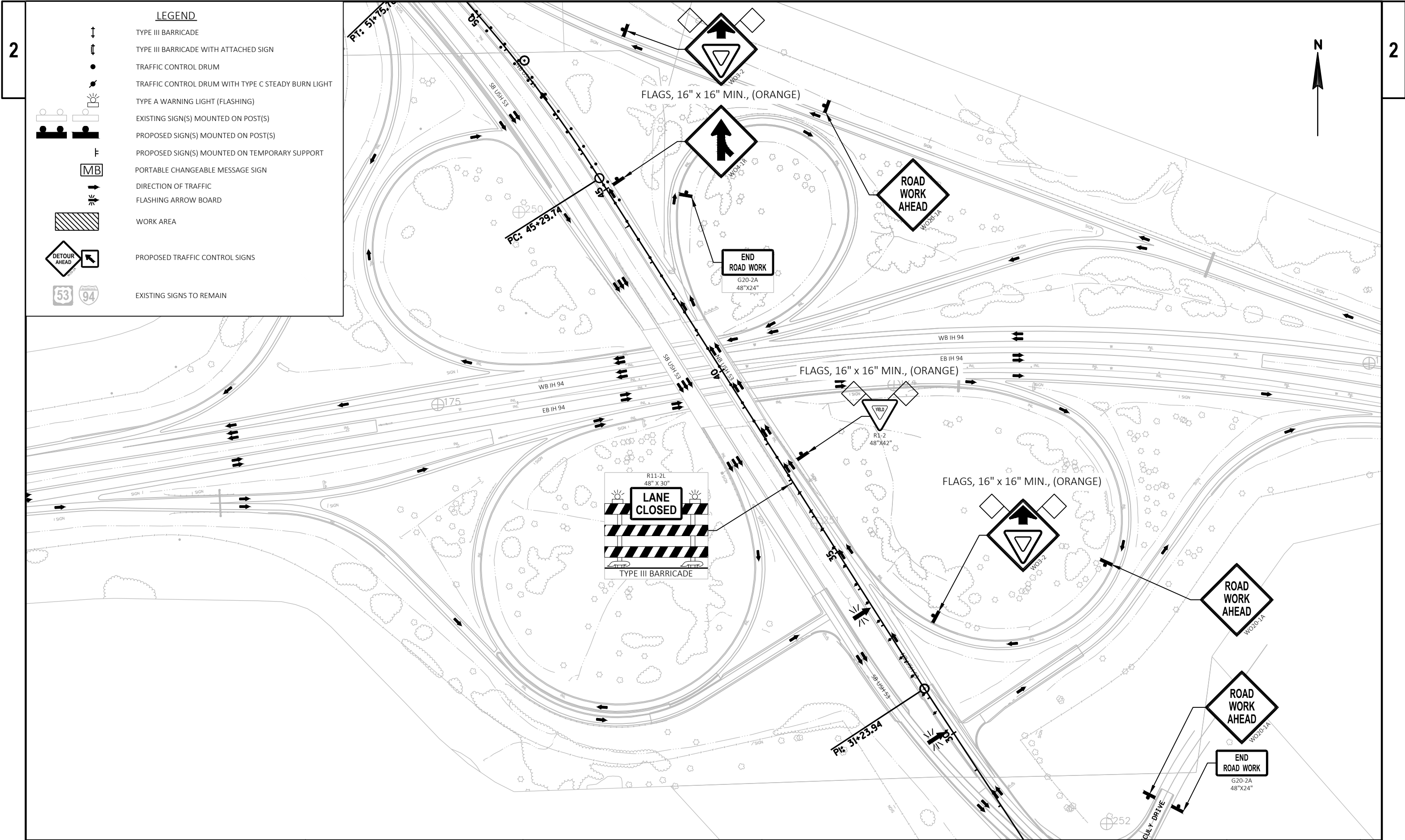
Compiled By:  
Date: 9/03  
Photography Da

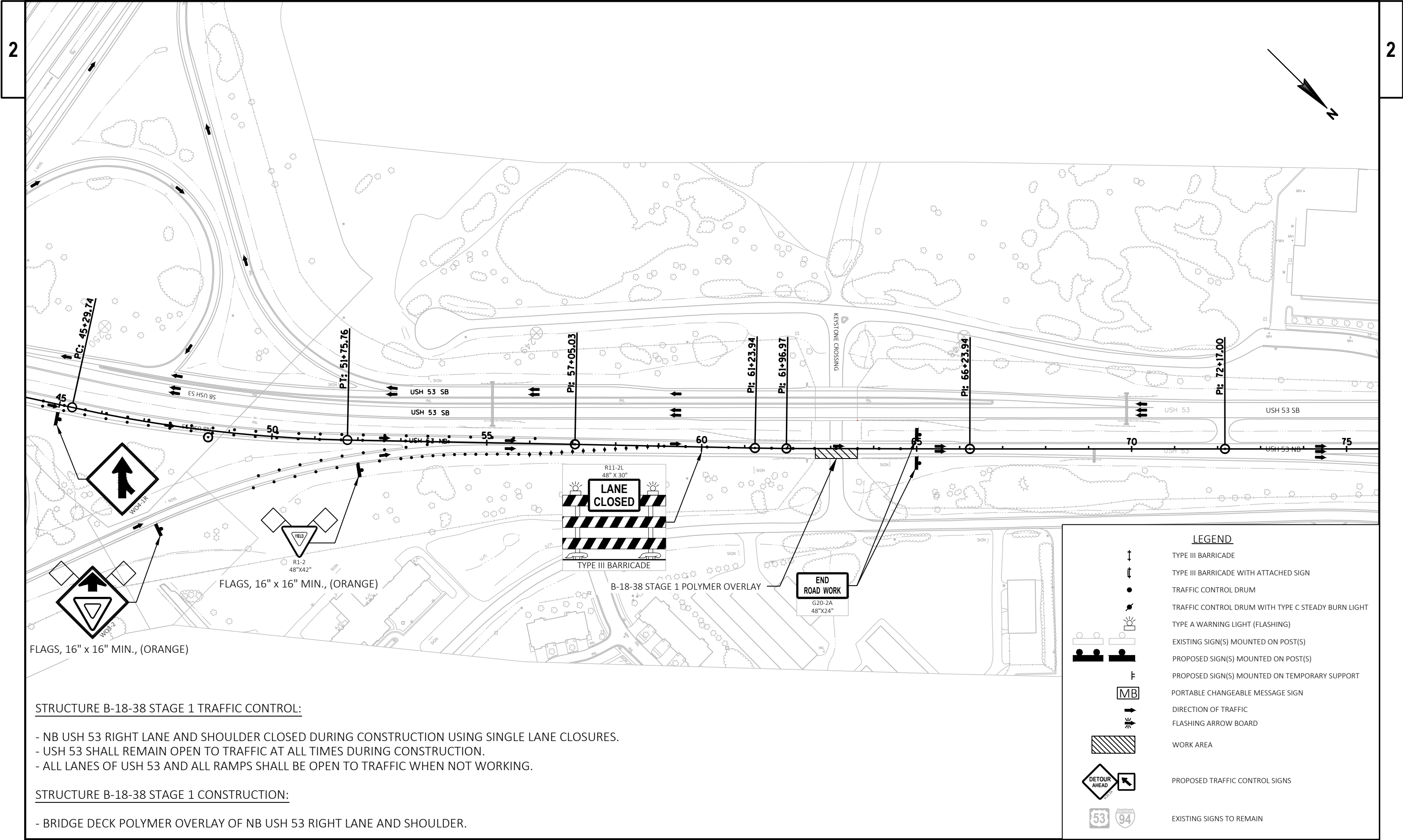


LEGEND

WORK AREA

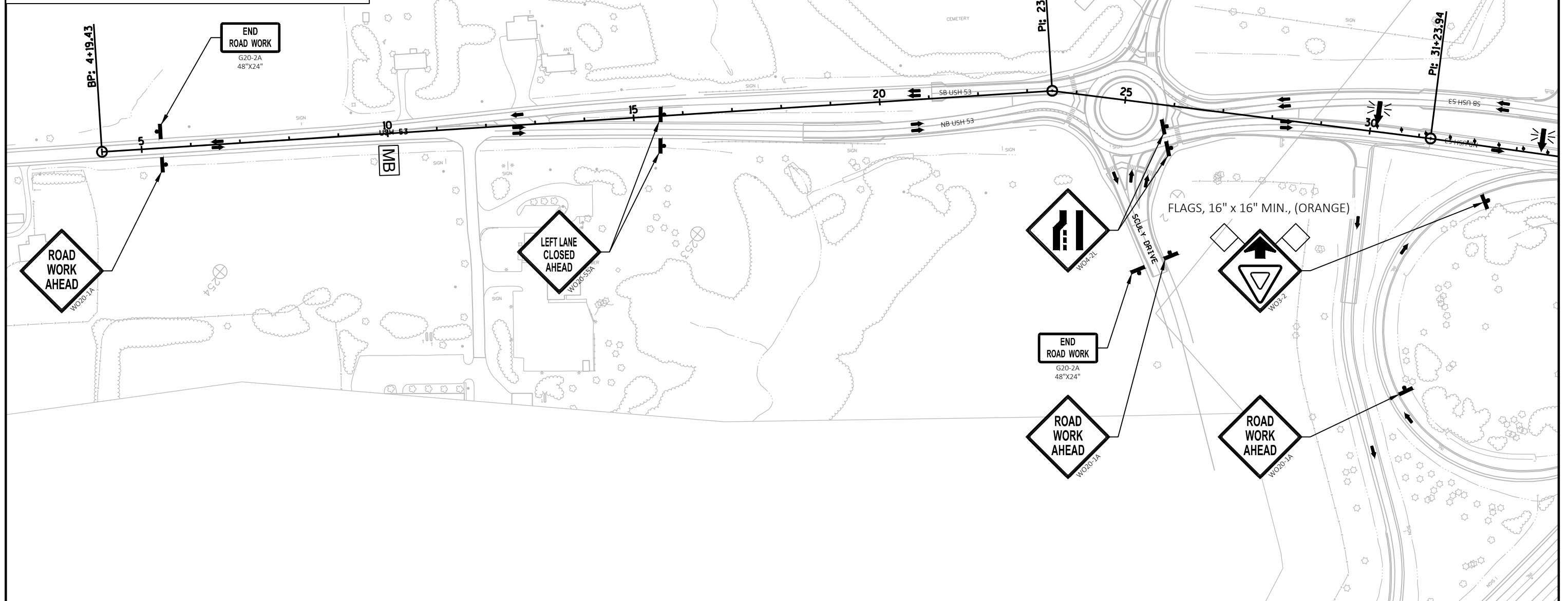
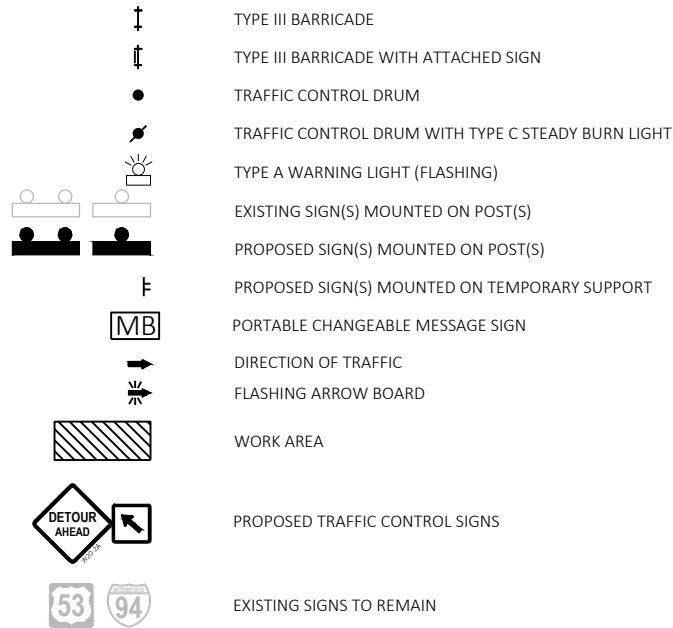








## LEGEND



PROJECT NO: 1190-02-64

HWY: USH 53

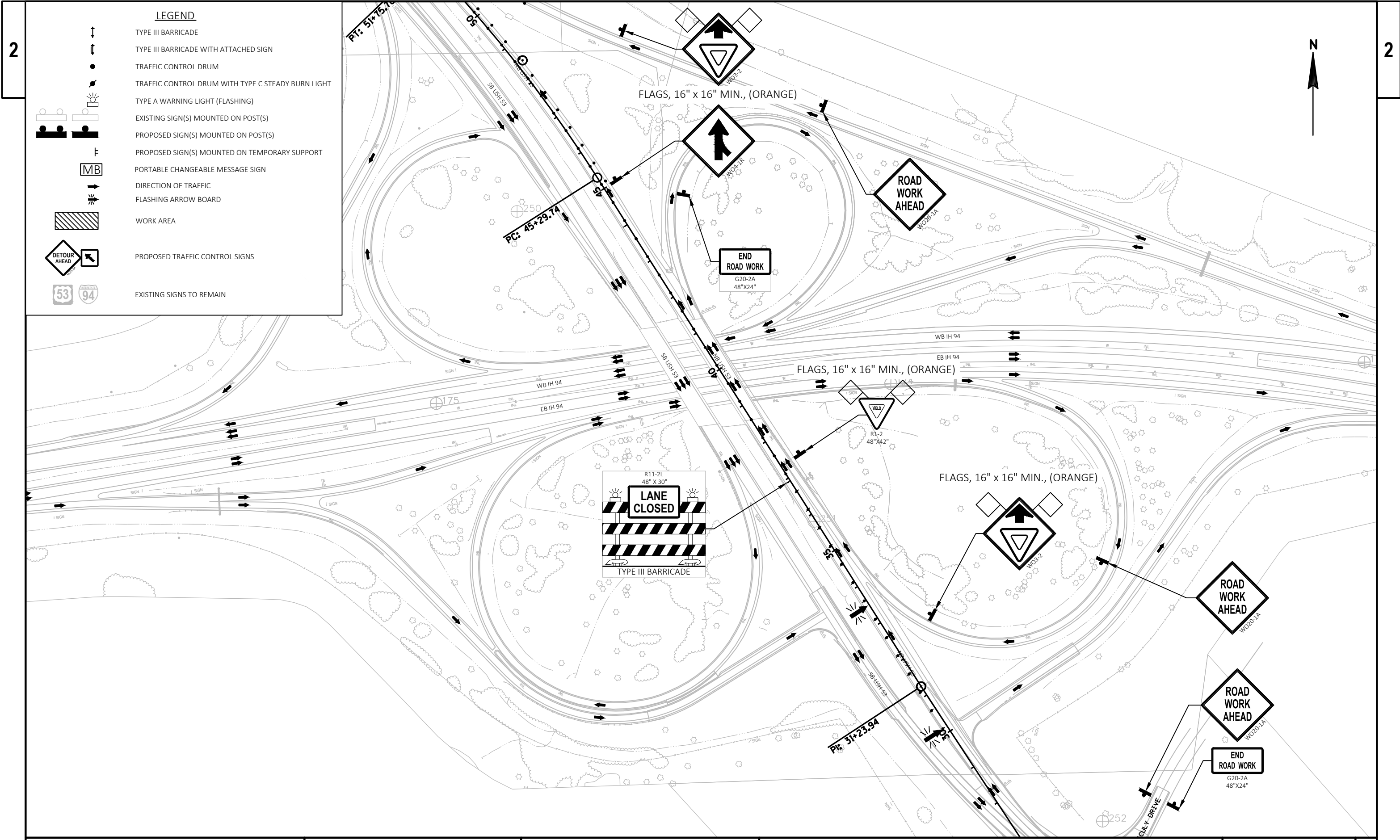
COUNTY: EAU CLAIRE

TRAFFIC CONTROL - B-18-38 STAGE 2 PLAN

SHEET

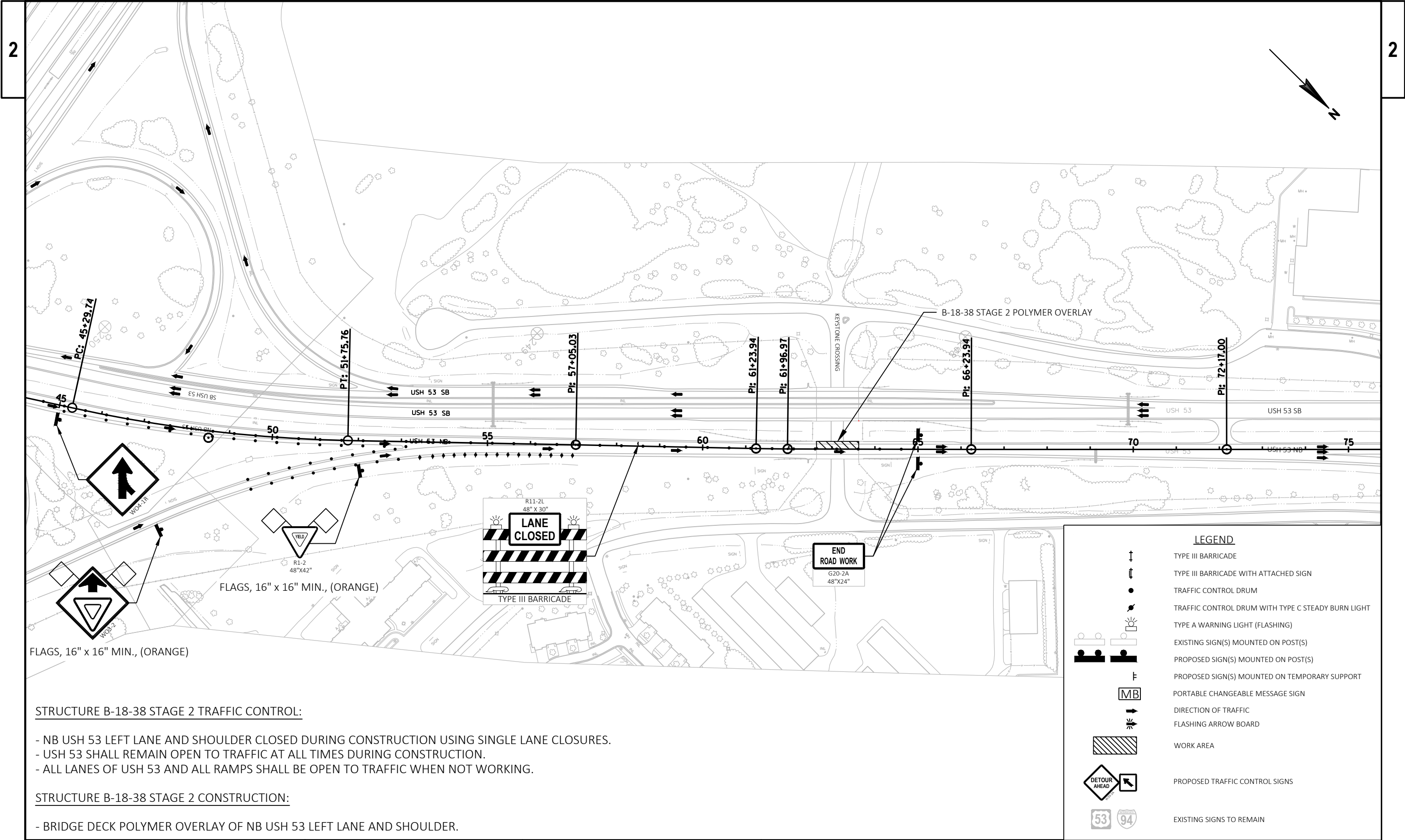
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2

2



STRUCTURE B-18-38 STAGE 2 TRAFFIC CONTROL:

- NB USH 53 LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- ALL LANES OF USH 53 AND ALL RAMPS SHALL BE OPEN TO TRAFFIC WHEN NOT WORKING.

STRUCTURE B-18-38 STAGE 2 CONSTRUCTION:

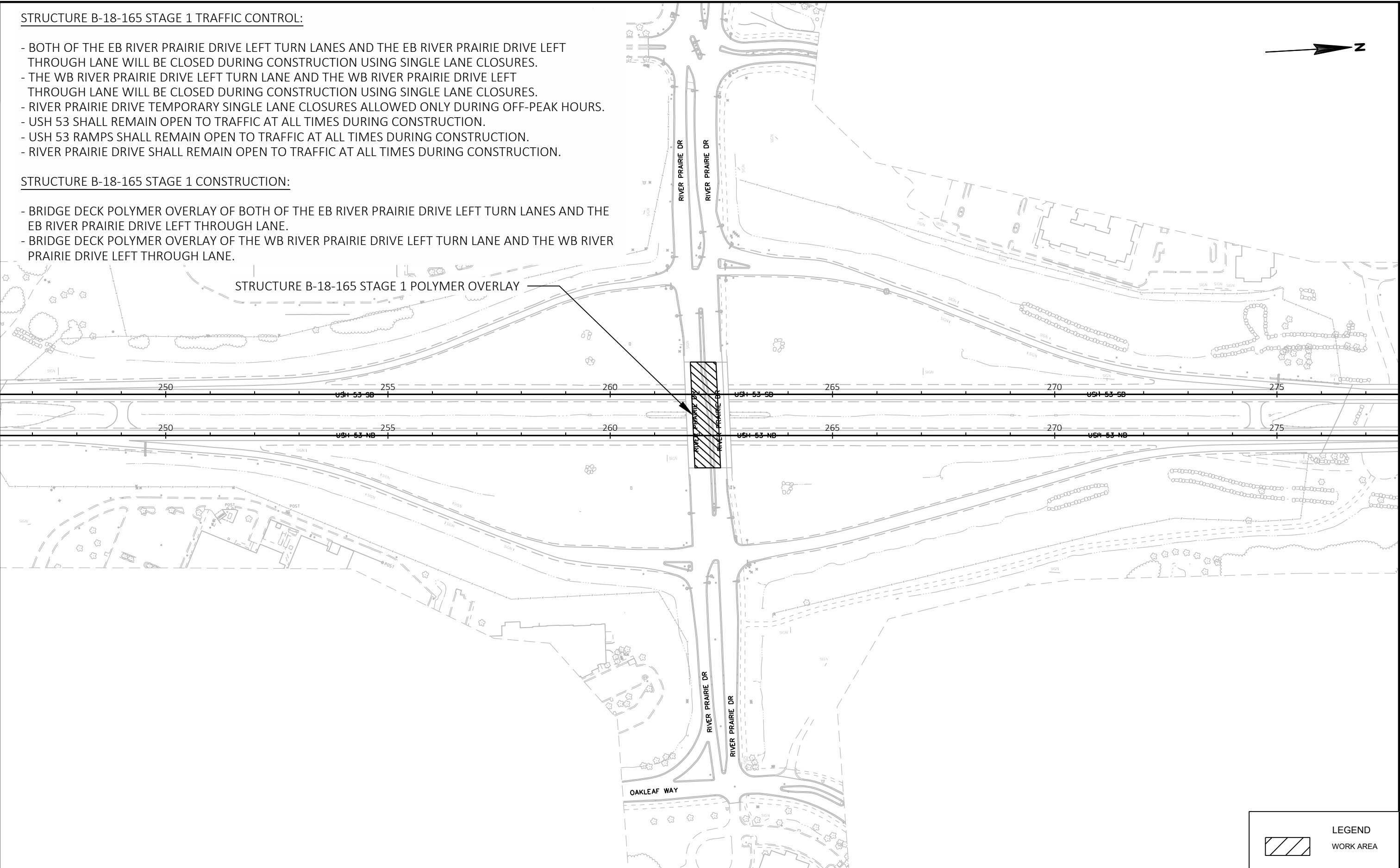
- BRIDGE DECK POLYMER OVERLAY OF NB USH 53 LEFT LANE AND SHOULDER.

STRUCTURE B-18-165 STAGE 1 TRAFFIC CONTROL:

- BOTH OF THE EB RIVER PRAIRIE DRIVE LEFT TURN LANES AND THE EB RIVER PRAIRIE DRIVE LEFT THROUGH LANE WILL BE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- THE WB RIVER PRAIRIE DRIVE LEFT TURN LANE AND THE WB RIVER PRAIRIE DRIVE LEFT THROUGH LANE WILL BE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- RIVER PRAIRIE DRIVE TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- USH 53 RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- RIVER PRAIRIE DRIVE SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-165 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF BOTH OF THE EB RIVER PRAIRIE DRIVE LEFT TURN LANES AND THE EB RIVER PRAIRIE DRIVE LEFT THROUGH LANE.
- BRIDGE DECK POLYMER OVERLAY OF THE WB RIVER PRAIRIE DRIVE LEFT TURN LANE AND THE WB RIVER PRAIRIE DRIVE LEFT THROUGH LANE.

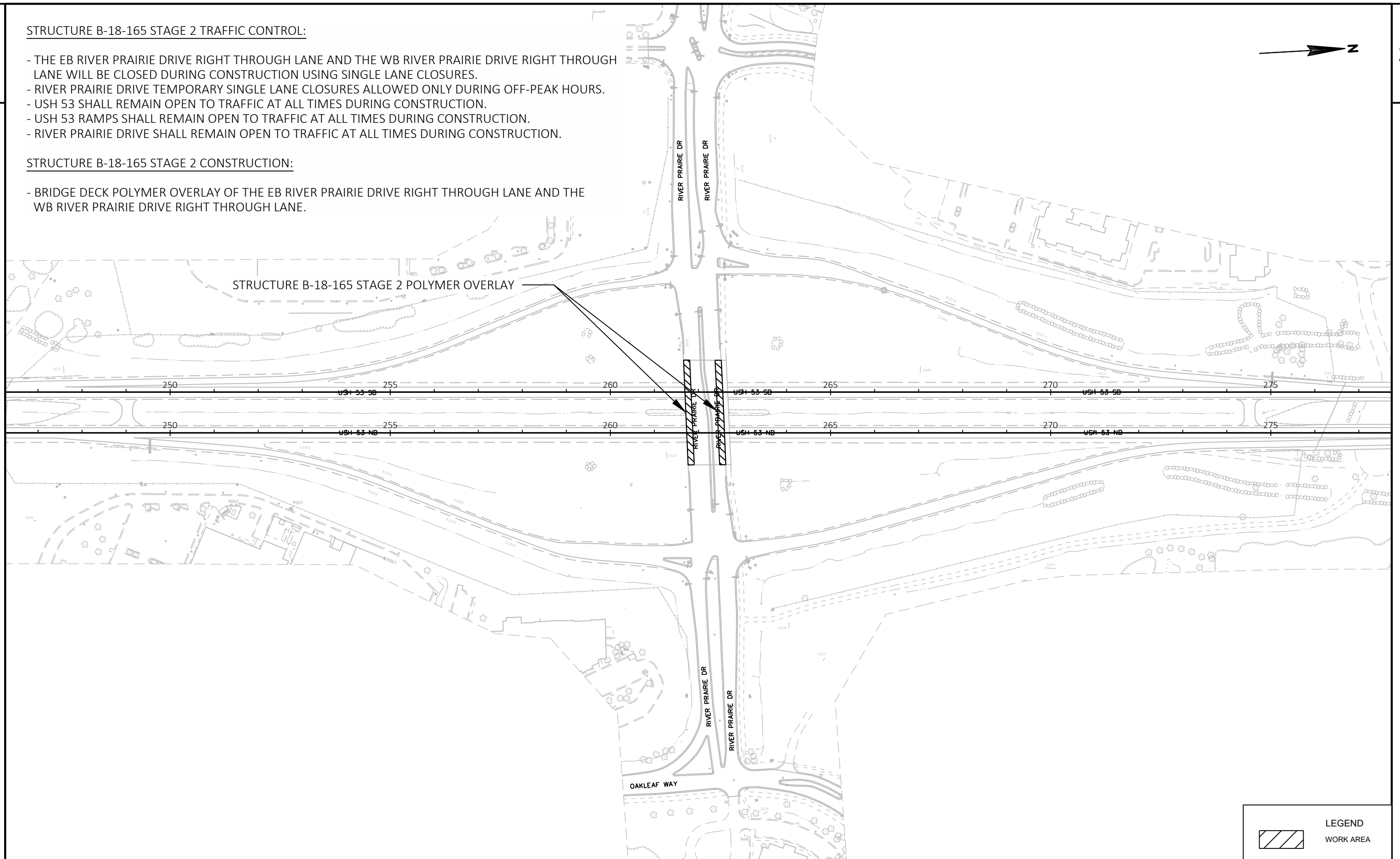


STRUCTURE B-18-165 STAGE 2 TRAFFIC CONTROL:

- THE EB RIVER PRAIRIE DRIVE RIGHT THROUGH LANE AND THE WB RIVER PRAIRIE DRIVE RIGHT THROUGH LANE WILL BE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- RIVER PRAIRIE DRIVE TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- USH 53 RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- RIVER PRAIRIE DRIVE SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-165 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF THE EB RIVER PRAIRIE DRIVE RIGHT THROUGH LANE AND THE WB RIVER PRAIRIE DRIVE RIGHT THROUGH LANE.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 2 OVERVIEW - B-18-165 RIVER PRAIRIE DRIVE

SHEET

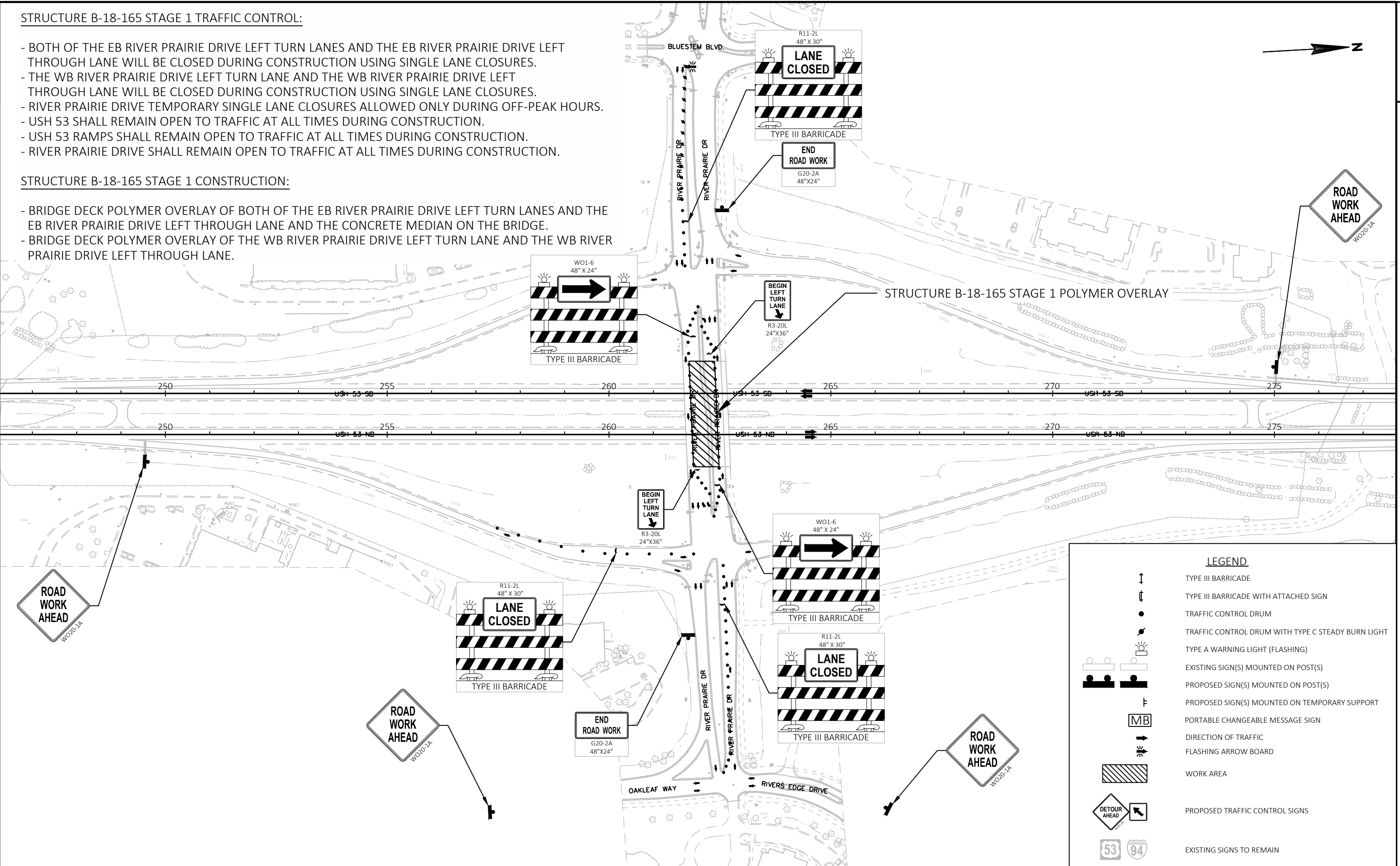
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STRUCTURE B-18-165 STAGE 1 TRAFFIC CONTROL:

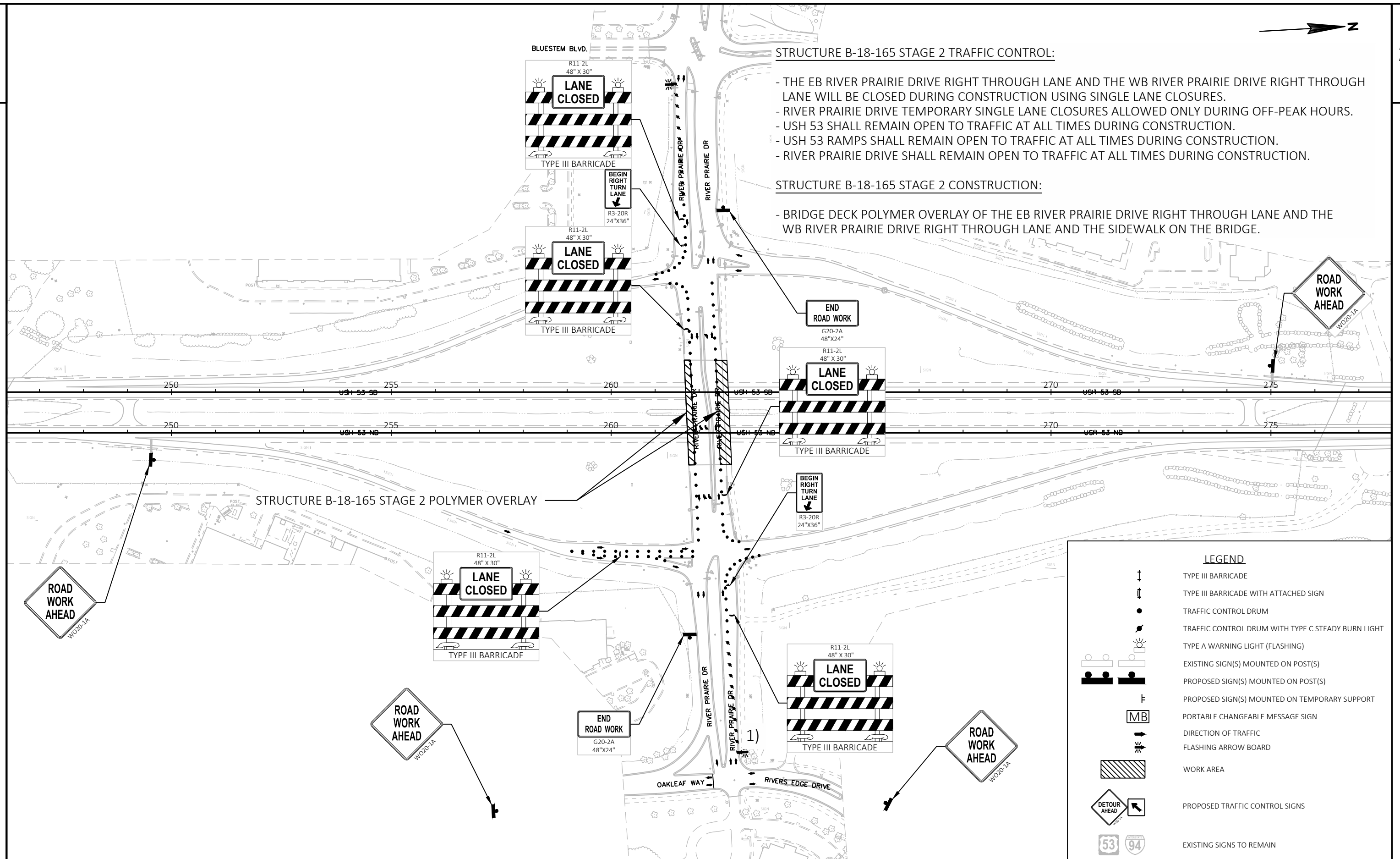
- BOTH OF THE EB RIVER PRAIRIE DRIVE LEFT TURN LANES AND THE EB RIVER PRAIRIE DRIVE LEFT THROUGH LANE WILL BE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- THE WB RIVER PRAIRIE DRIVE LEFT TURN LANE AND THE WB RIVER PRAIRIE DRIVE LEFT THROUGH LANE WILL BE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- RIVER PRAIRIE DRIVE TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- USH 53 RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.
- RIVER PRAIRIE DRIVE SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-165 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF BOTH OF THE EB RIVER PRAIRIE DRIVE LEFT TURN LANES AND THE EB RIVER PRAIRIE DRIVE LEFT THROUGH LANE AND THE CONCRETE MEDIAN ON THE BRIDGE.
- BRIDGE DECK POLYMER OVERLAY OF THE WB RIVER PRAIRIE DRIVE LEFT TURN LANE AND THE WB RIVER PRAIRIE DRIVE LEFT THROUGH LANE.







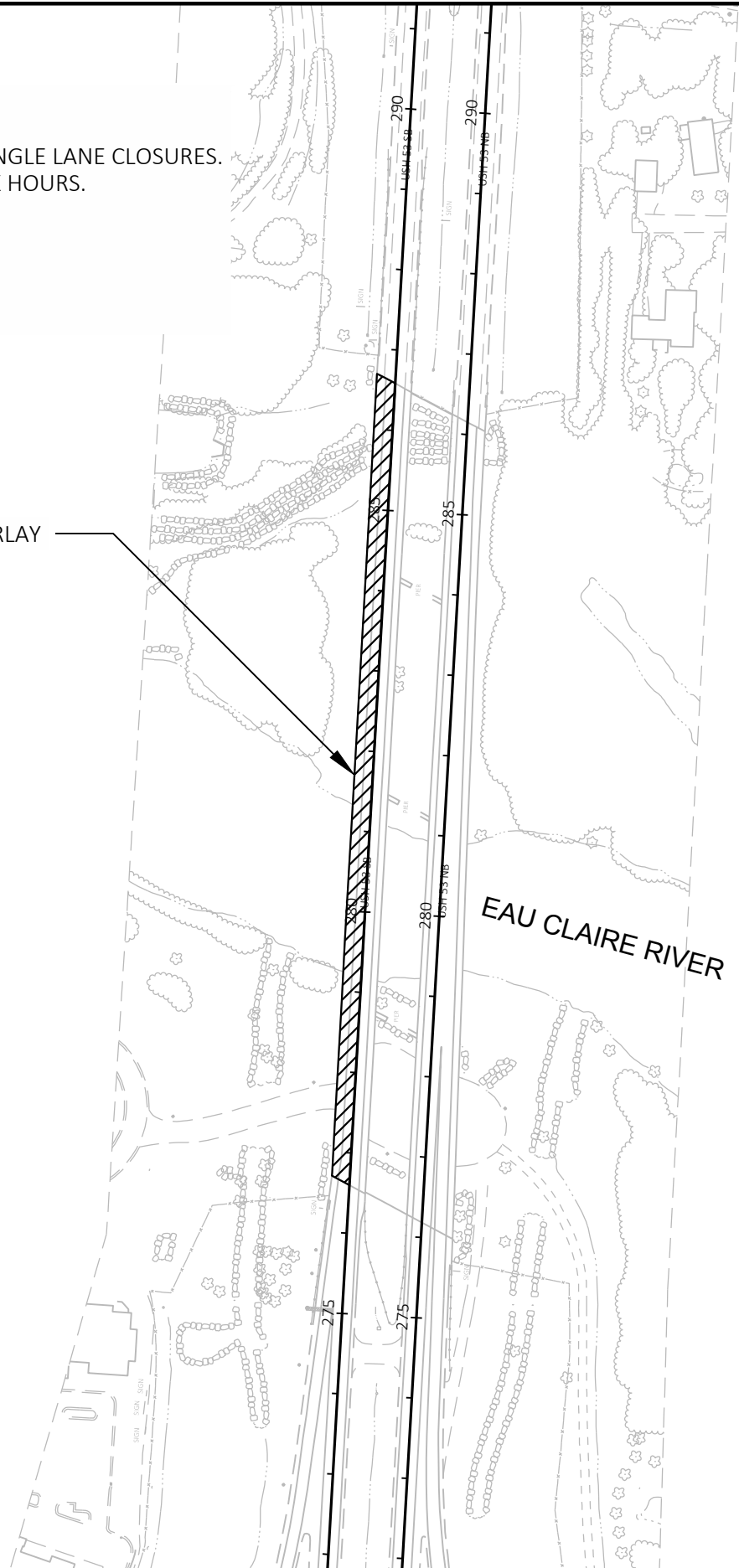
STRUCTURE B-18-168 STAGE 1 TRAFFIC CONTROL:

- USH 53 SB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-168 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB RIGHT LANE AND SHOULDER.

STRUCTURE B-18-168 STAGE 1 POLYMER OVERLAY



LEGEND  
WORK AREA

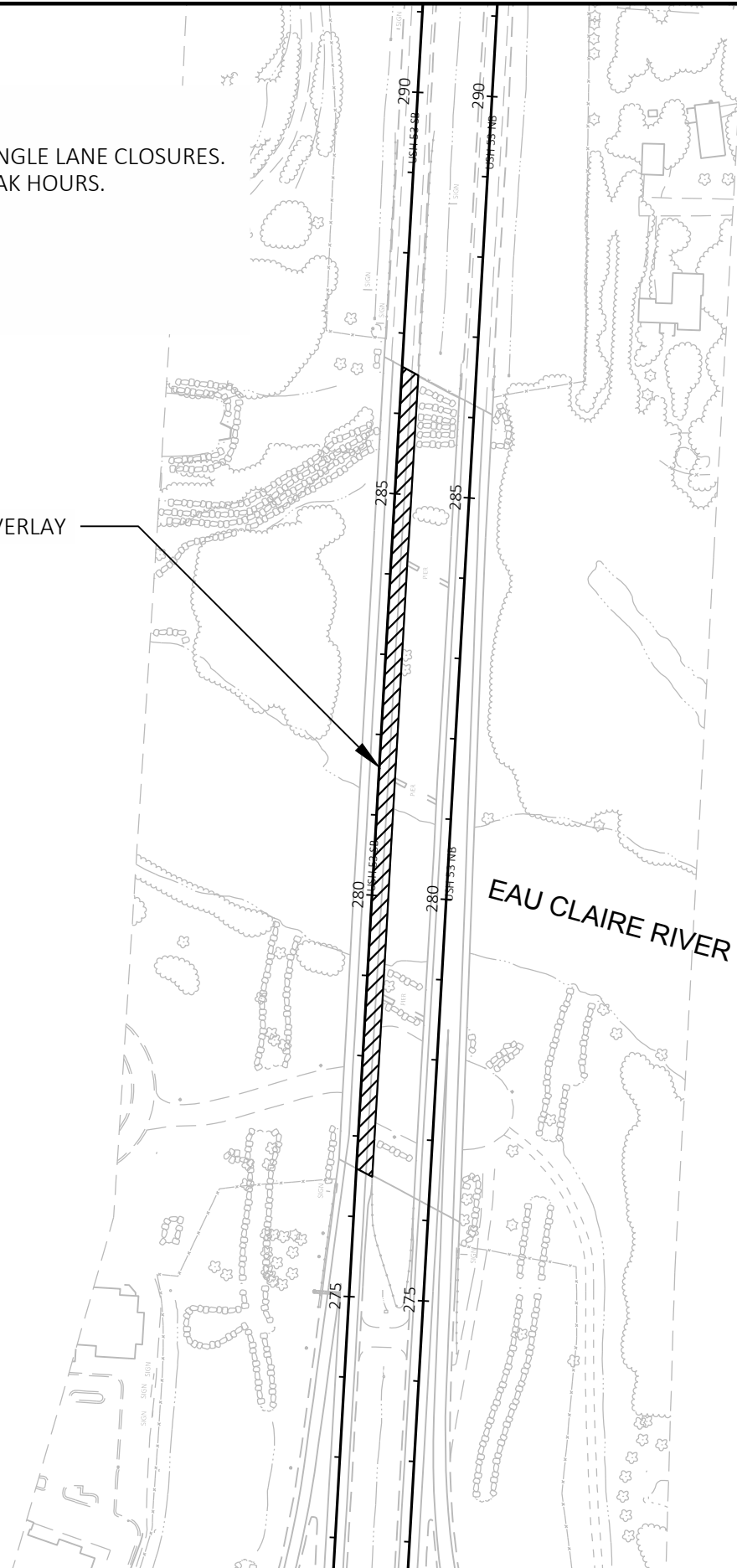
STRUCTURE B-18-168 STAGE 2 TRAFFIC CONTROL:

- USH 53 SB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-168 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB LEFT LANE AND SHOULDER.

STRUCTURE B-18-168 STAGE 2 POLYMER OVERLAY



LEGEND  
WORK AREA

PROJECT NO: 1190-02-64

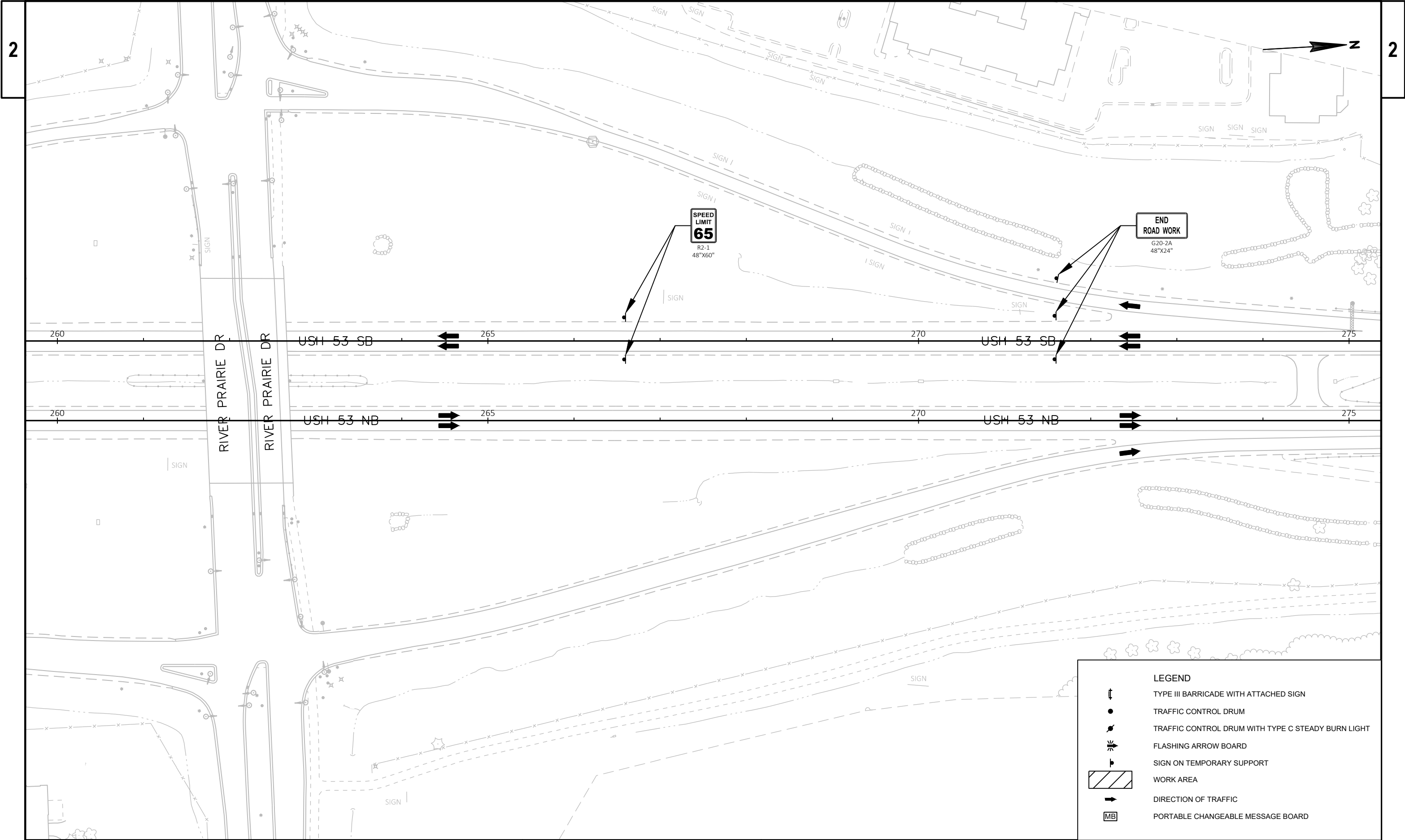
HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 2 OVERVIEW - B-18-168 EAU CLAIRE RIVER

SHEET

E

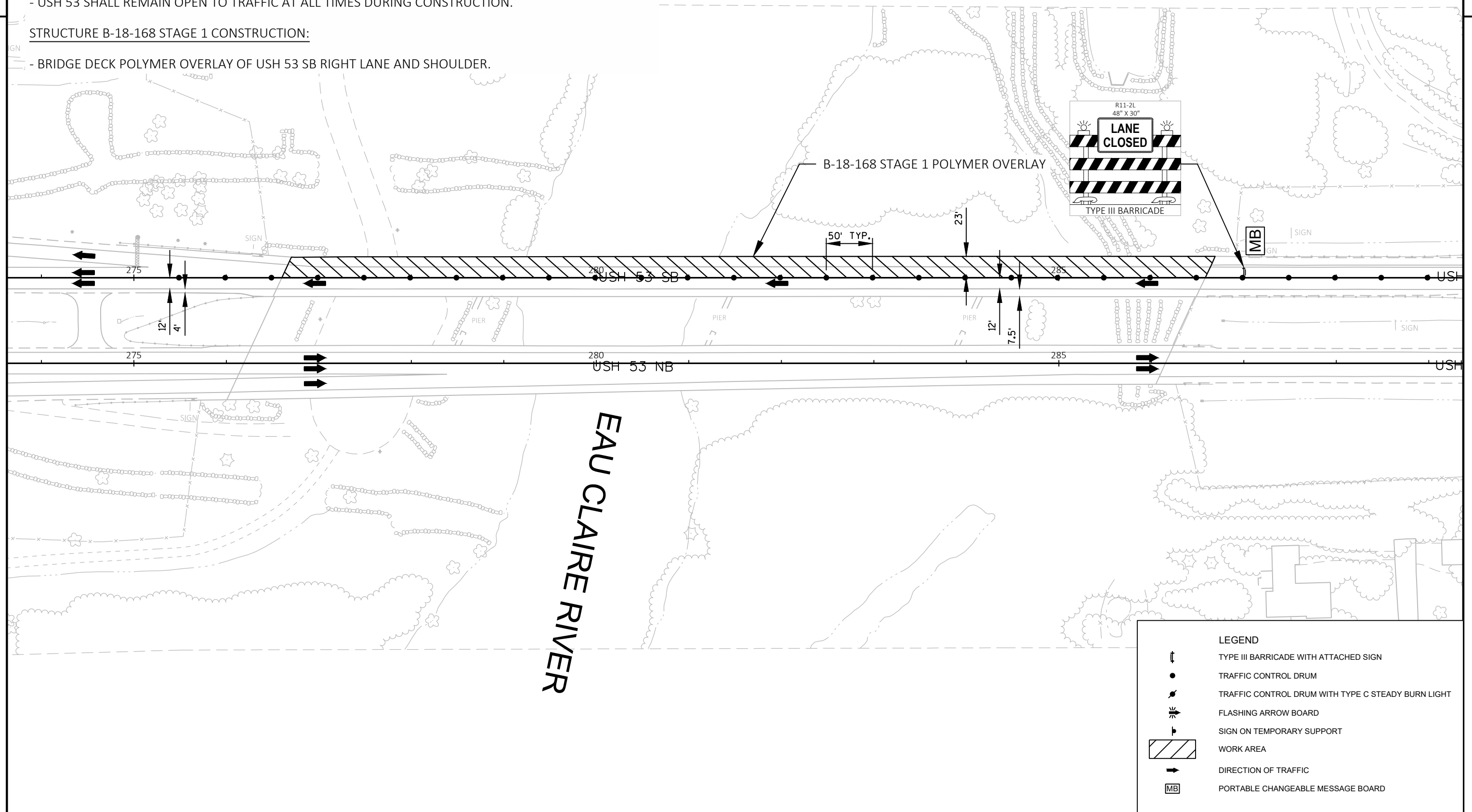


STRUCTURE B-18-168 STAGE 1 TRAFFIC CONTROL:

- USH 53 SB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-168 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB RIGHT LANE AND SHOULDER.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 1 - B-18-168 EAU CLAIRE RIVER

SHEET

E

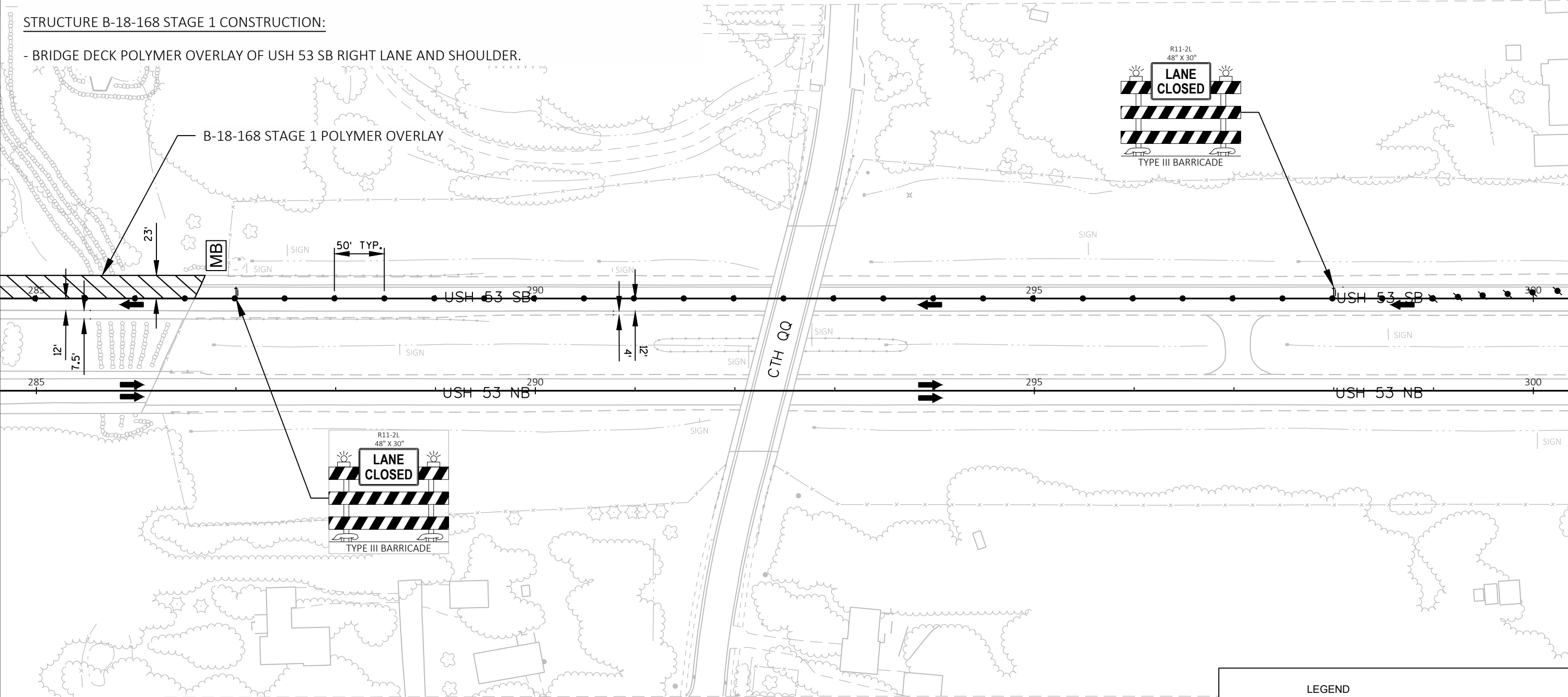


STRUCTURE B-18-168 STAGE 1 TRAFFIC CONTROL:

- USH 53 SB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

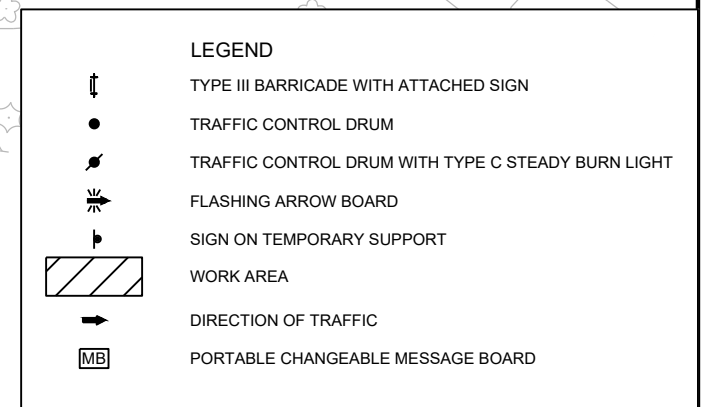
STRUCTURE B-18-168 STAGE 1 CONSTRUCTION:

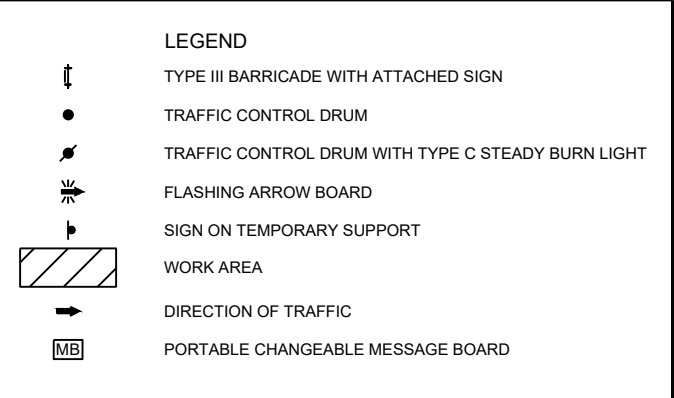
- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB RIGHT LANE AND SHOULDER.

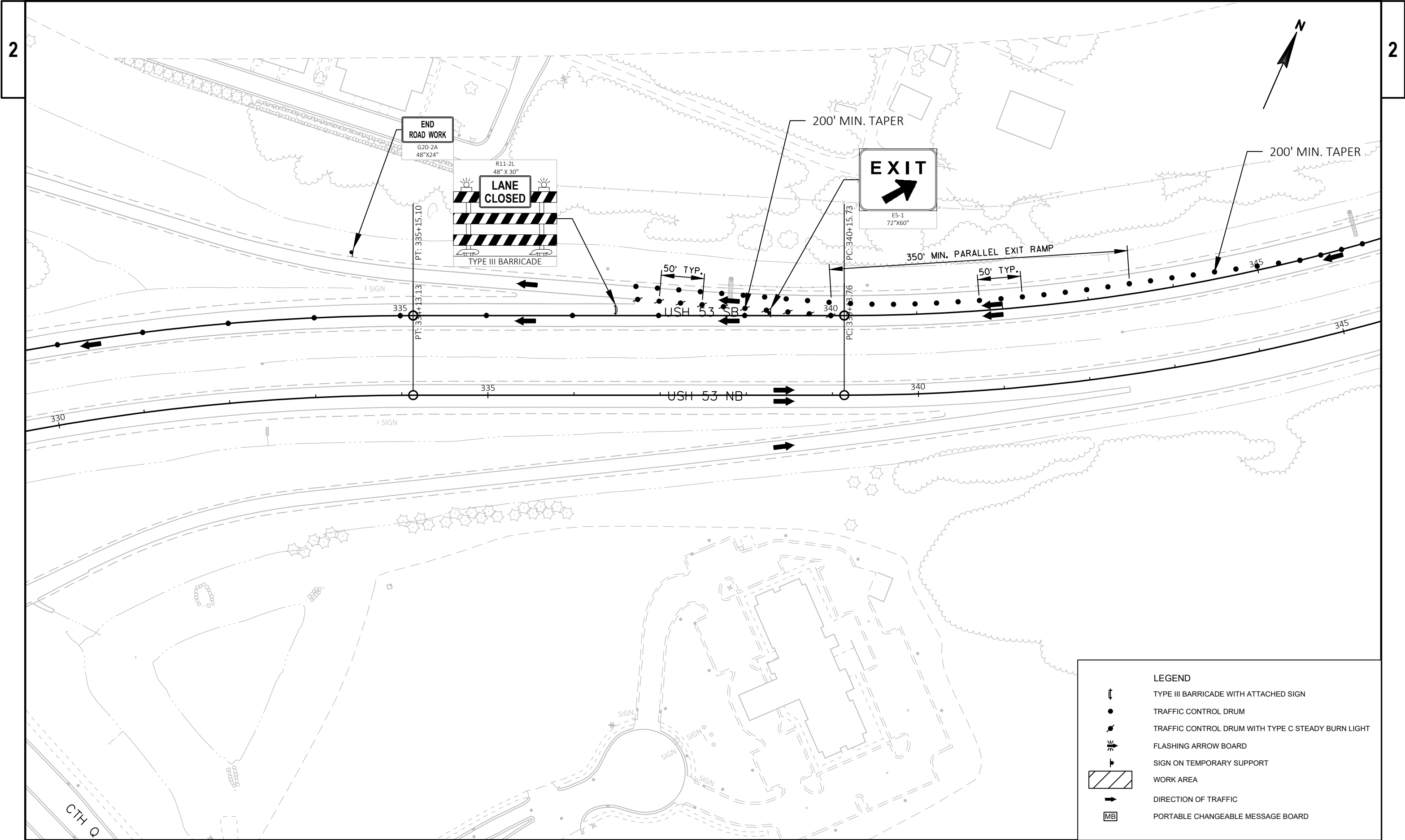


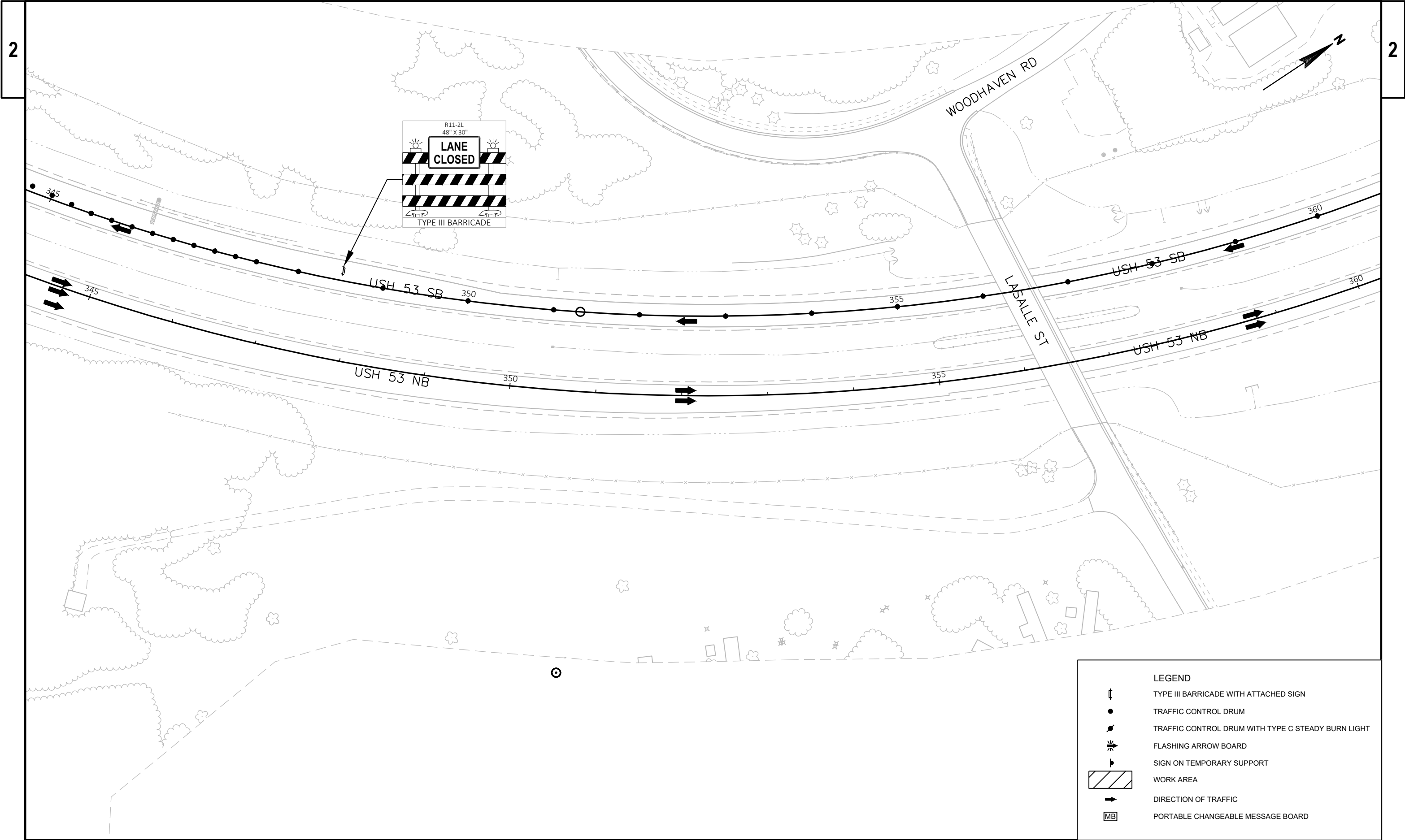
**LEGEND**

- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- DIRECTION OF TRAFFIC
- PORTABLE CHANGEABLE MESSAGE BOARD

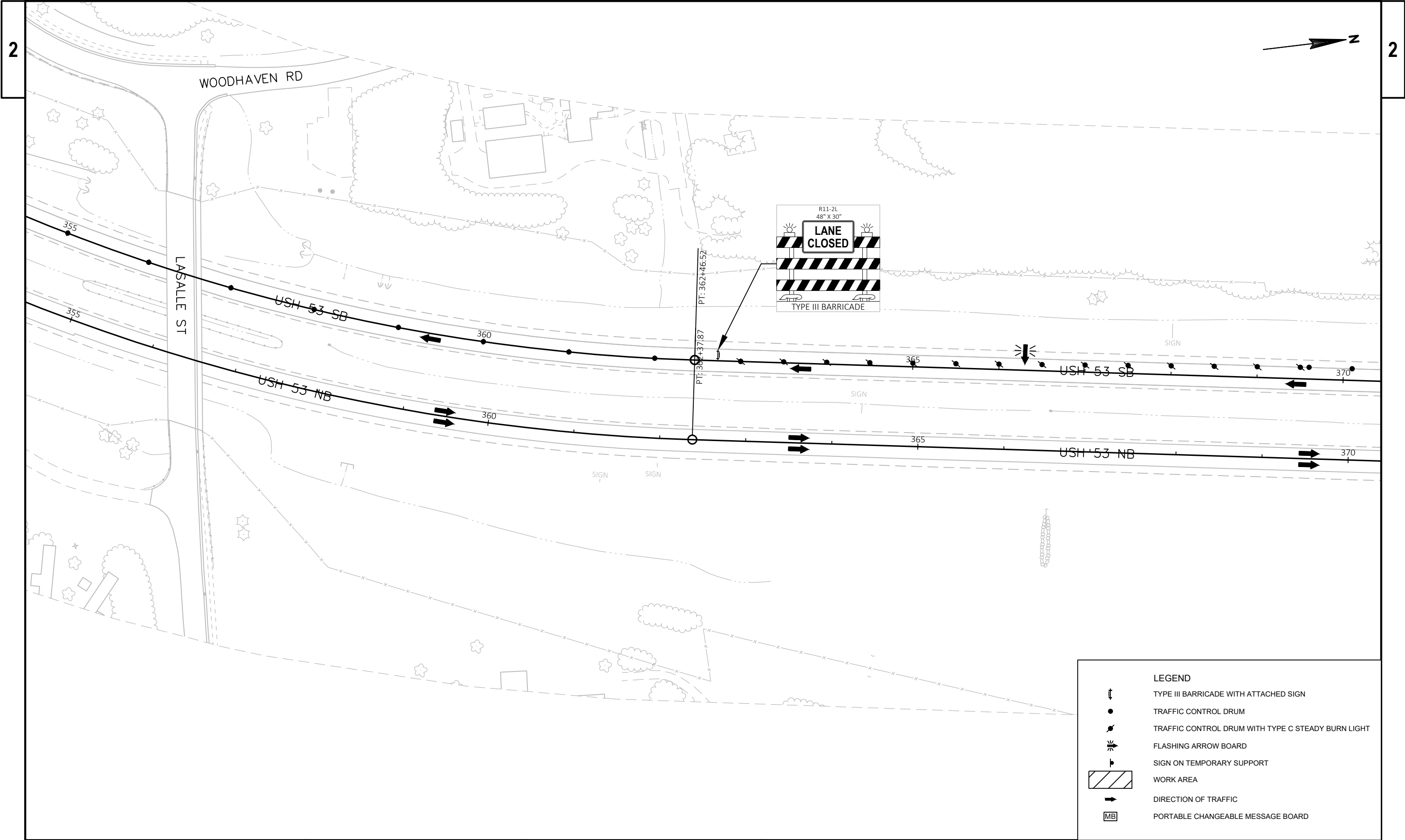


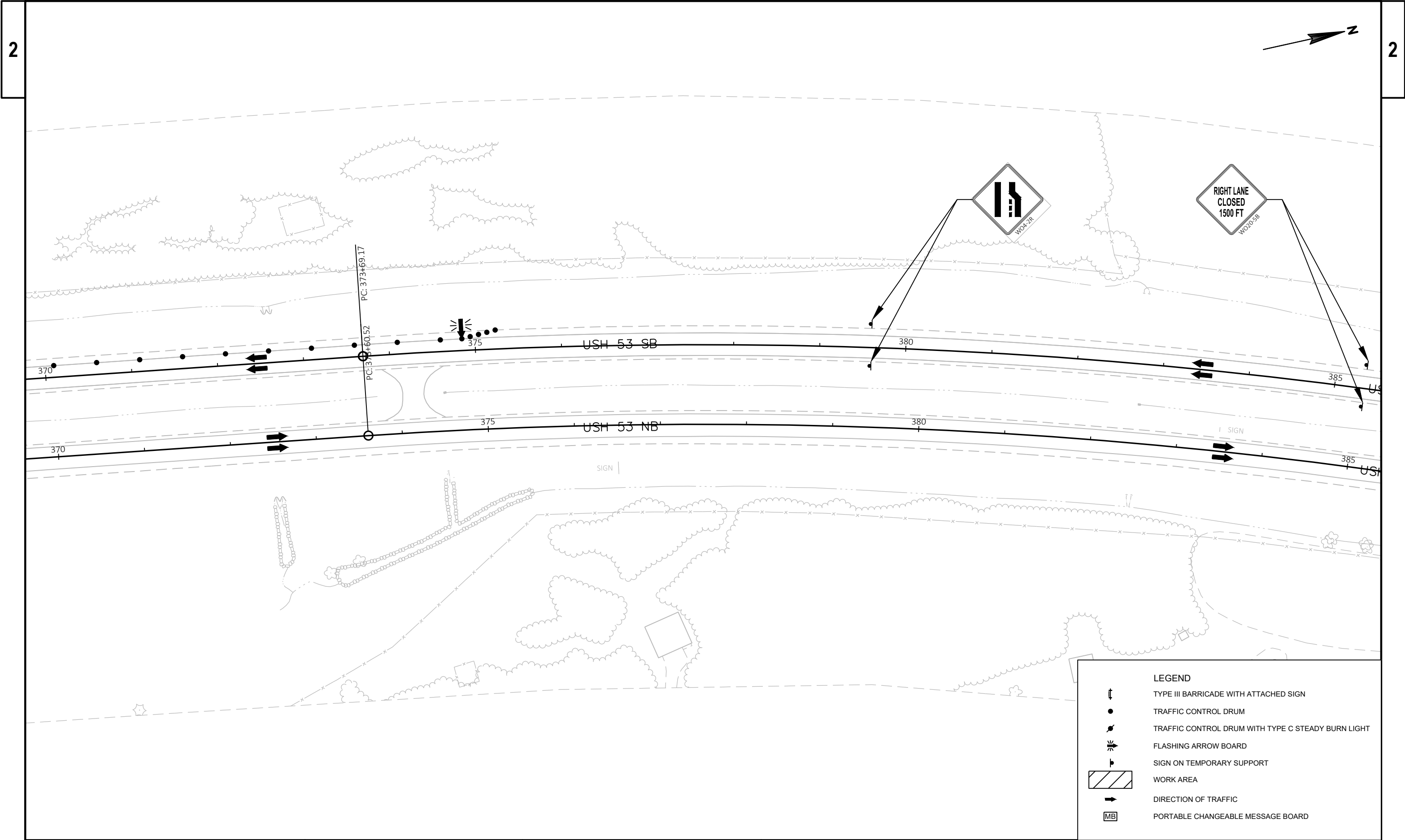




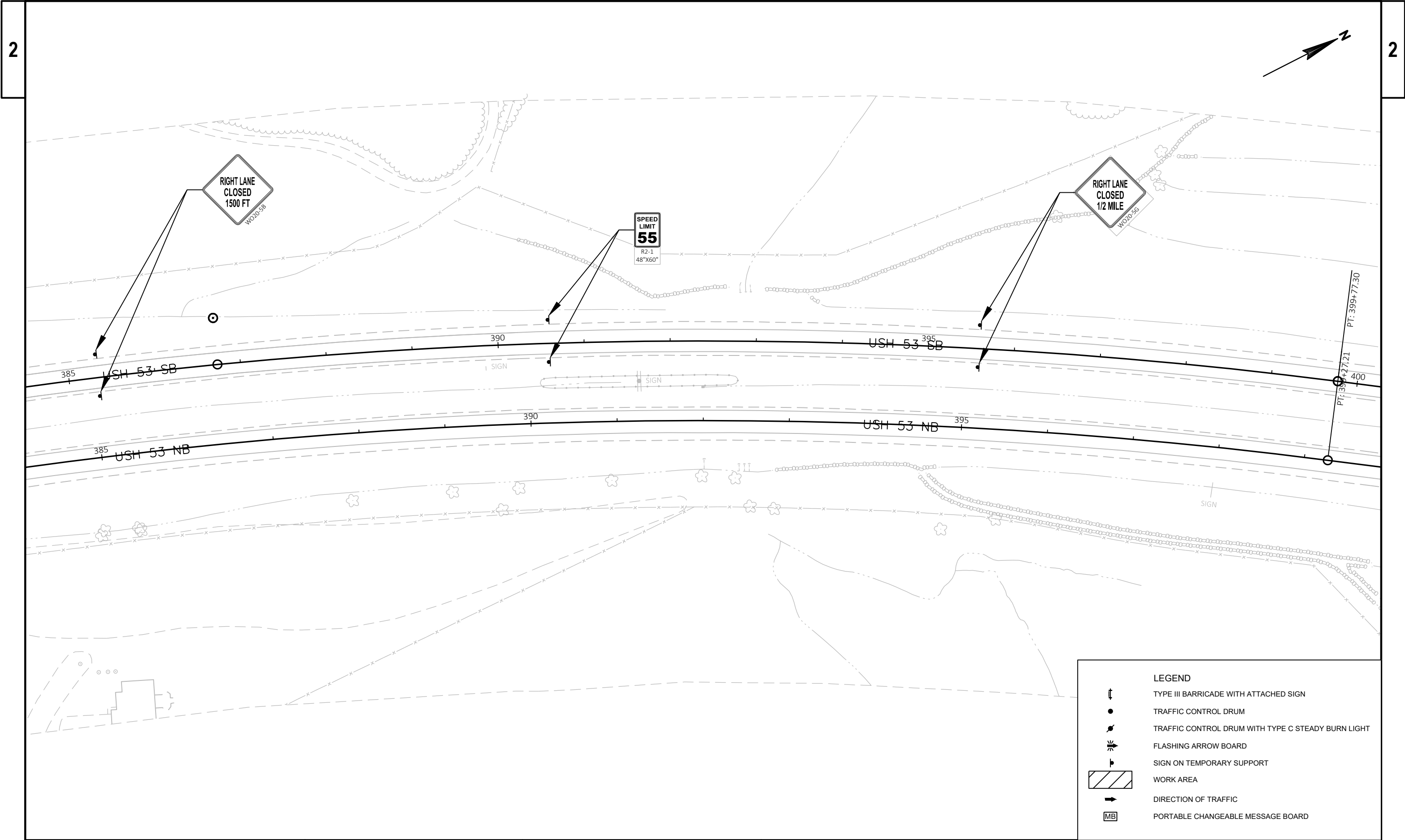


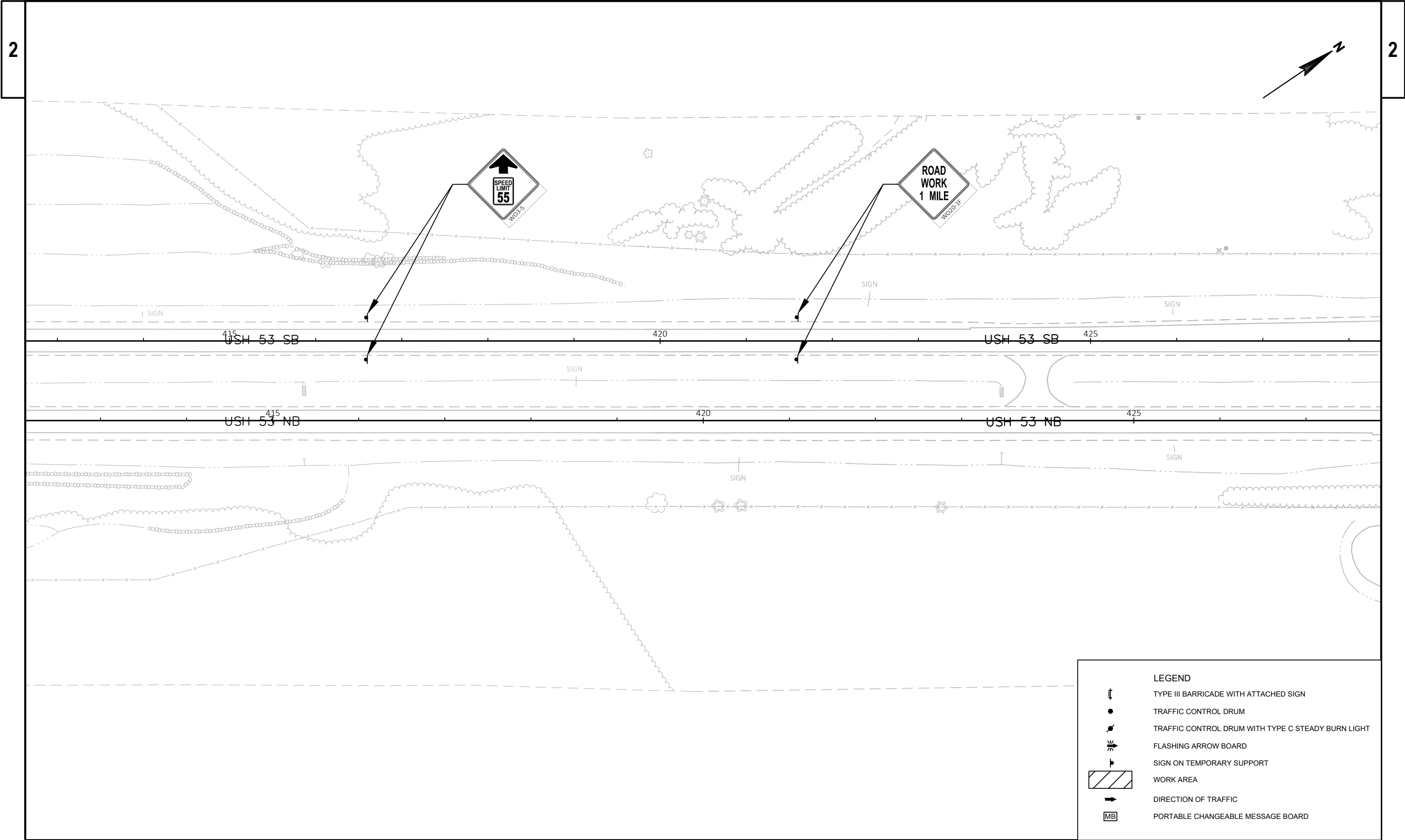


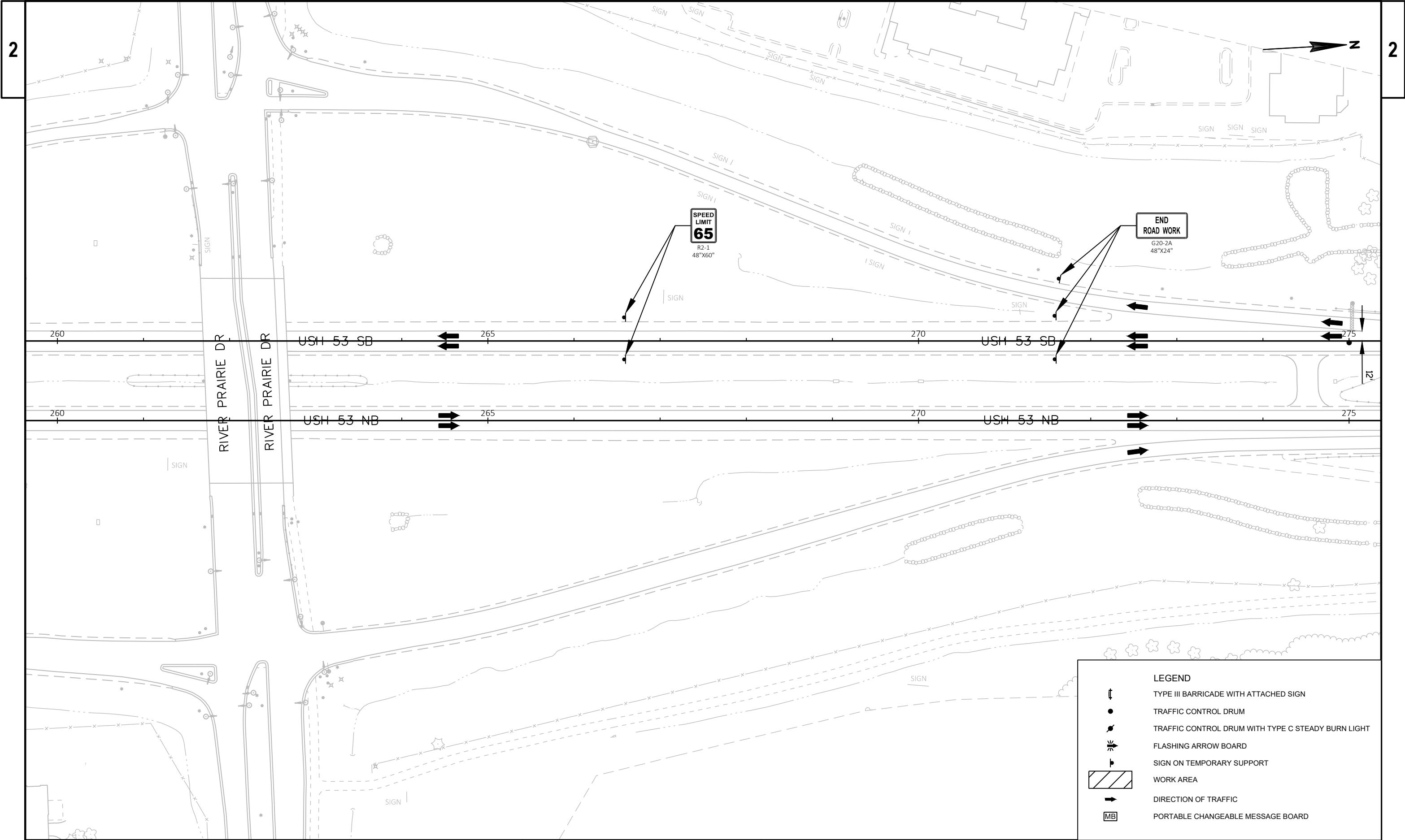




PROJECT NO: 1190-02-64	HWY: USH 53	COUNTY: EAU CLAIRE	TRAFFIC CONTROL - STAGE 1 - B-18-168 EAU CLAIRE RIVER	SHEET	E
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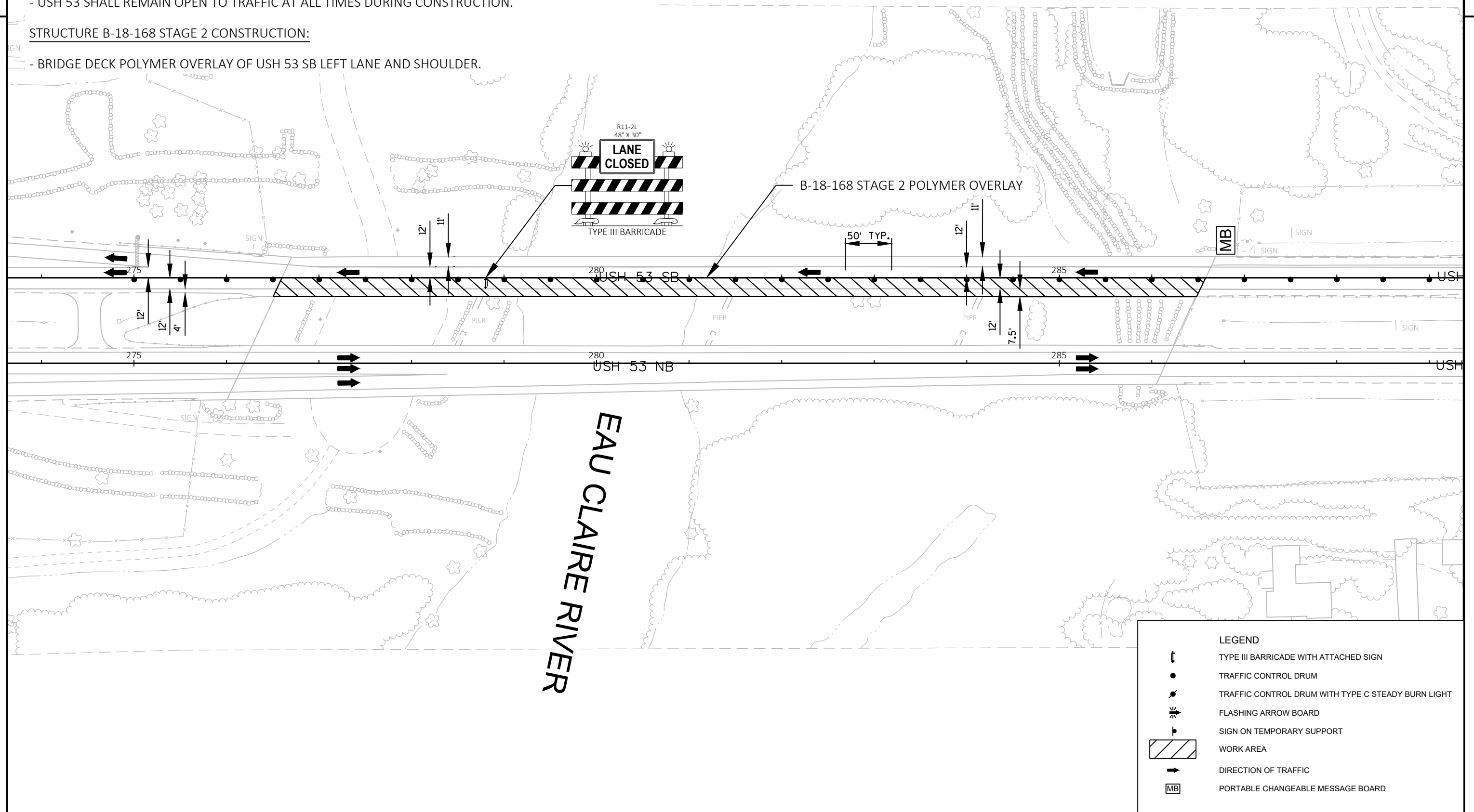
# 2

STRUCTURE B-18-168 STAGE 2 TRAFFIC CONTROL:

- USH 53 SB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-168 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB LEFT LANE AND SHOULDER.



PROJECT NO:	1190-02-64
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HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 2 - B-18-168 EAU CLAIRE RIVER

SHEET

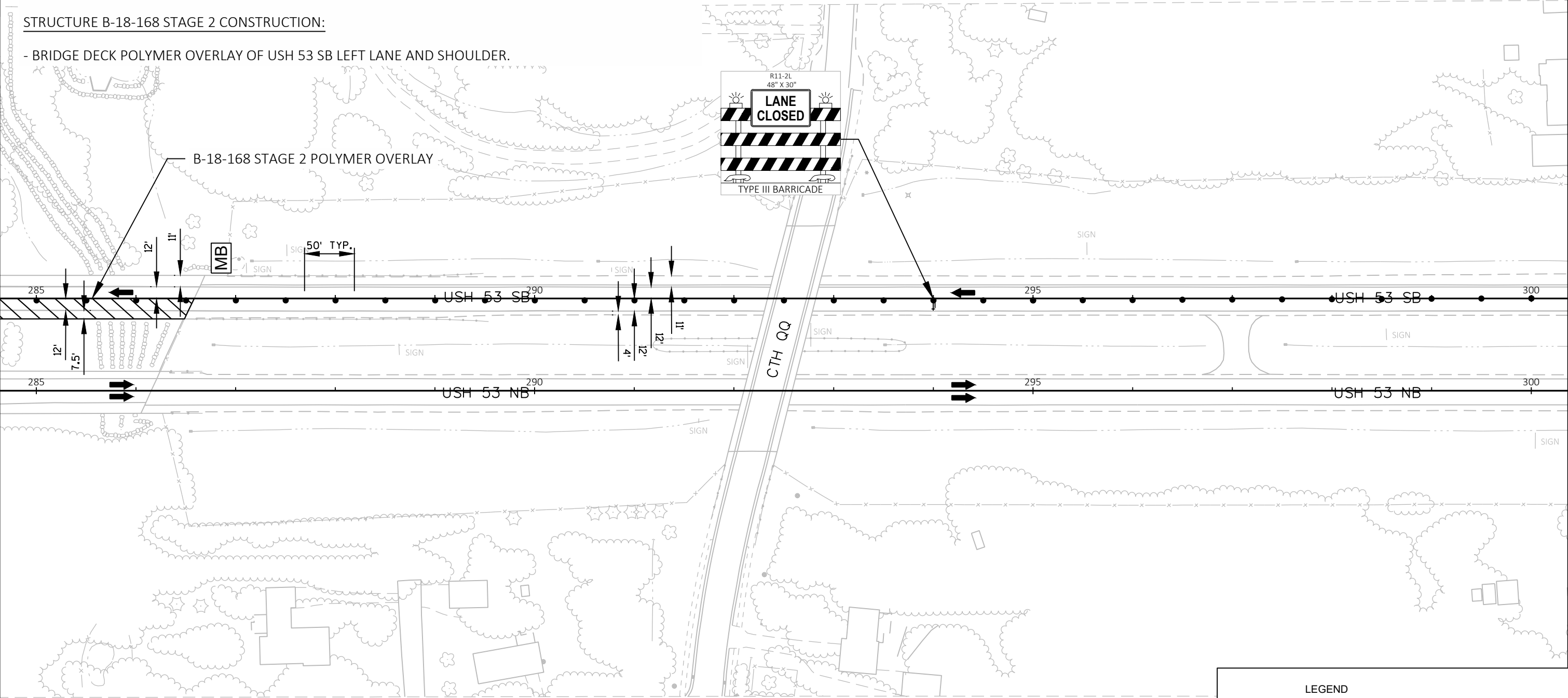
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STRUCTURE B-18-168 STAGE 2 TRAFFIC CONTROL:

- USH 53 SB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-168 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB LEFT LANE AND SHOULDER.



LEGEND

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

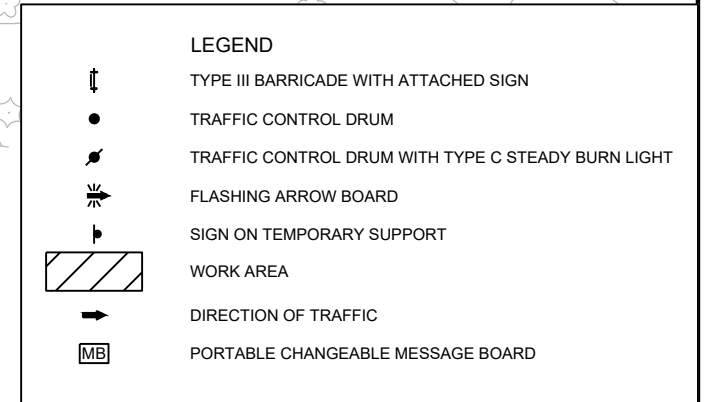
FLASHING ARROW BOARD

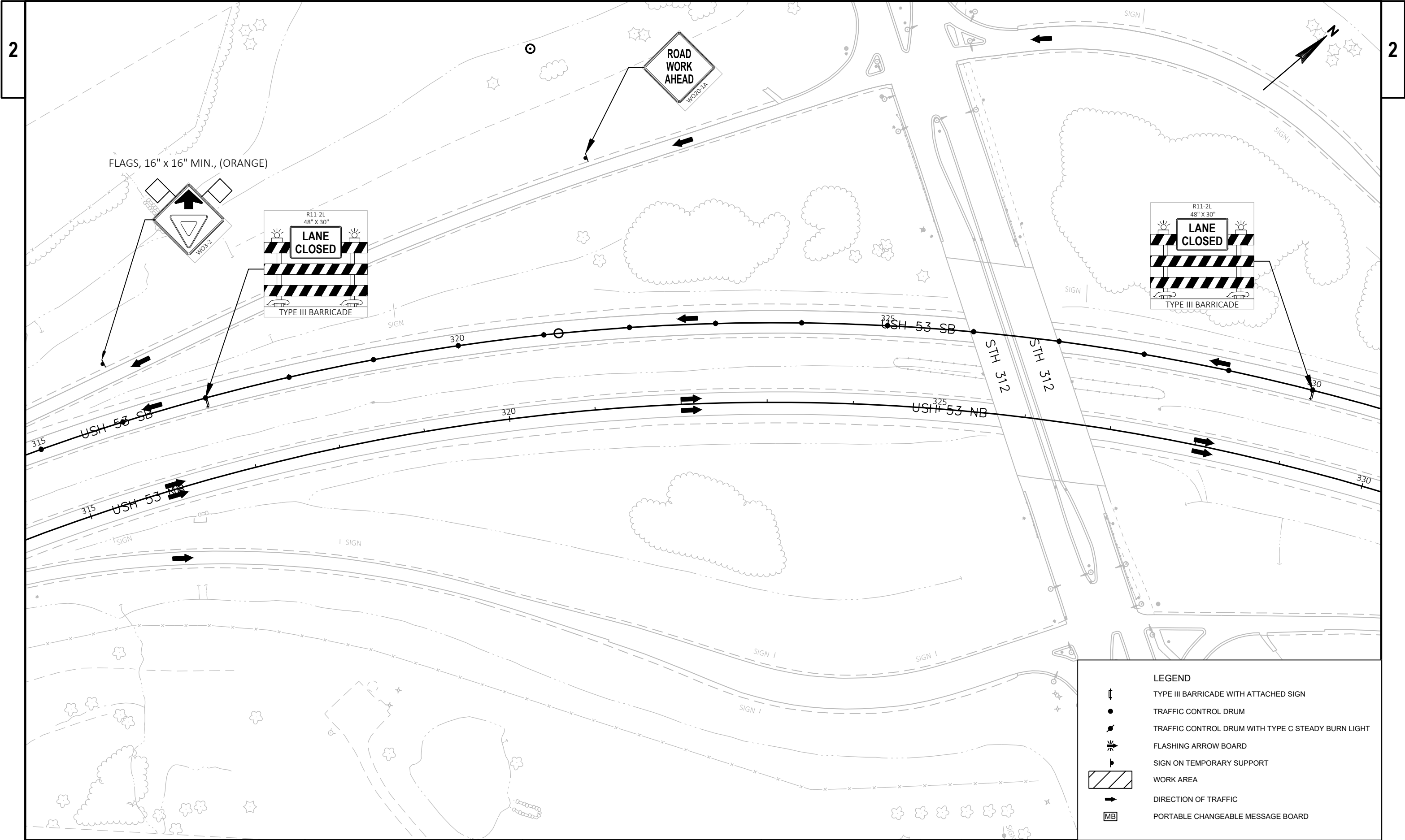
SIGN ON TEMPORARY SUPPORT

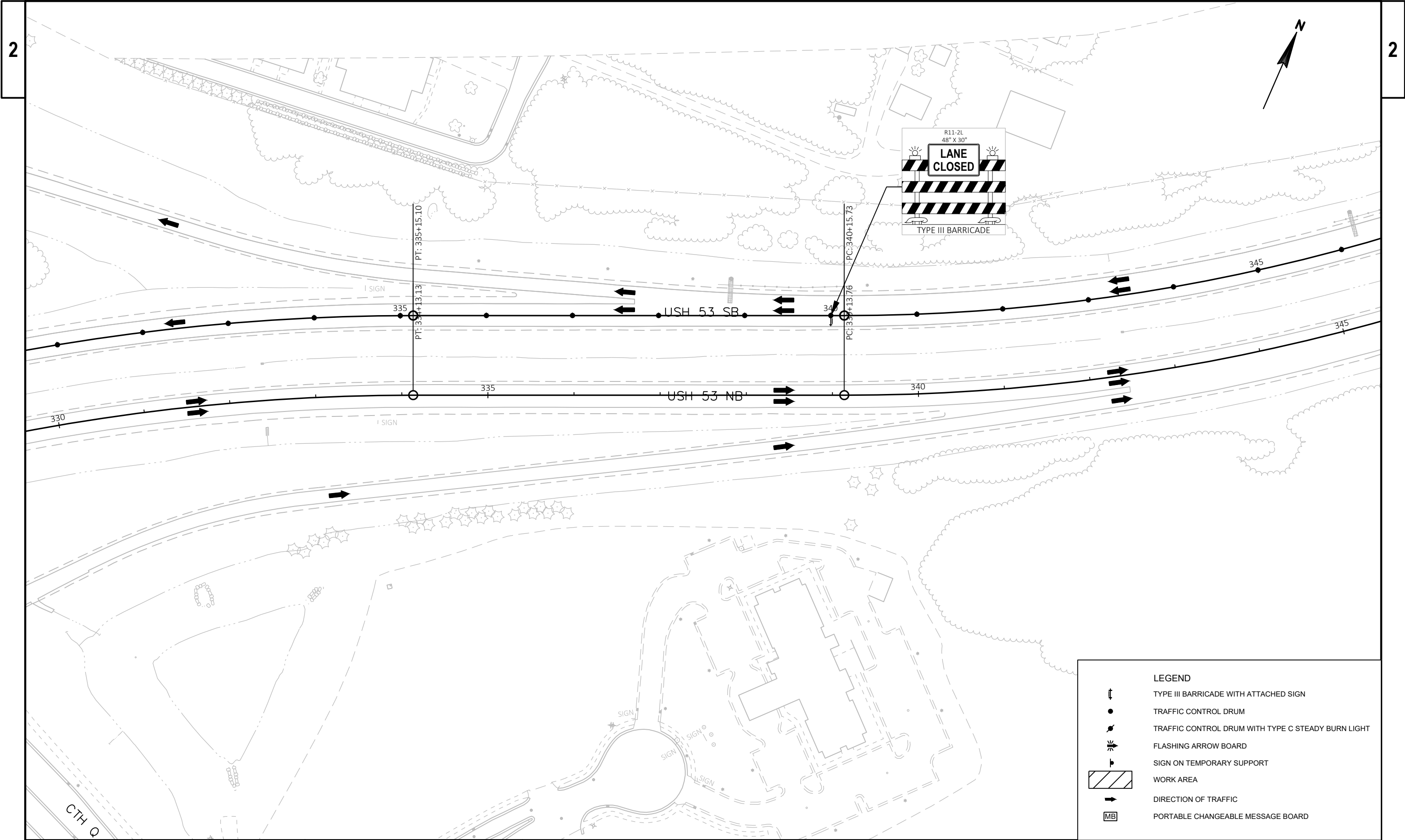
WORK AREA

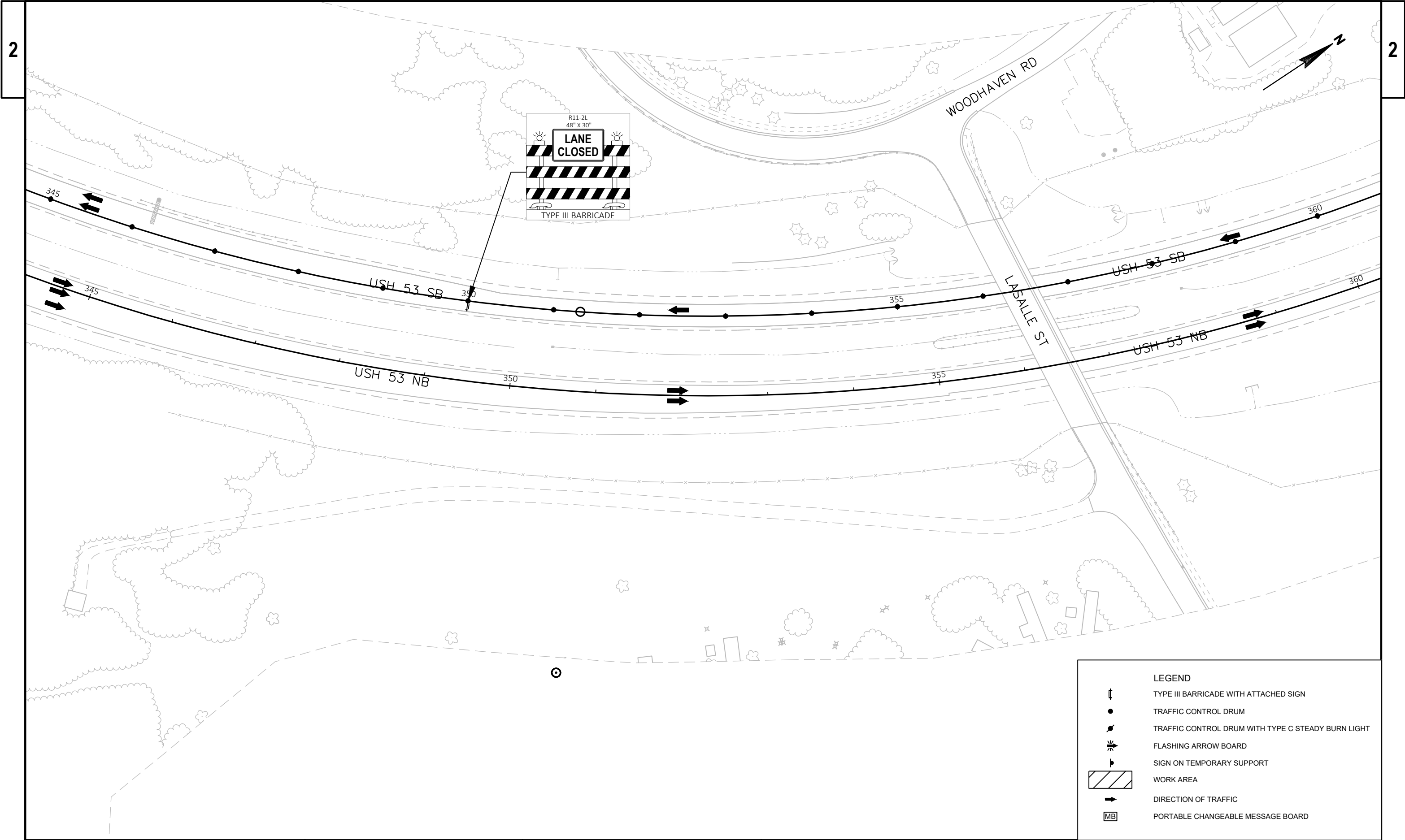
DIRECTION OF TRAFFIC

PORTABLE CHANGEABLE MESSAGE BOARD

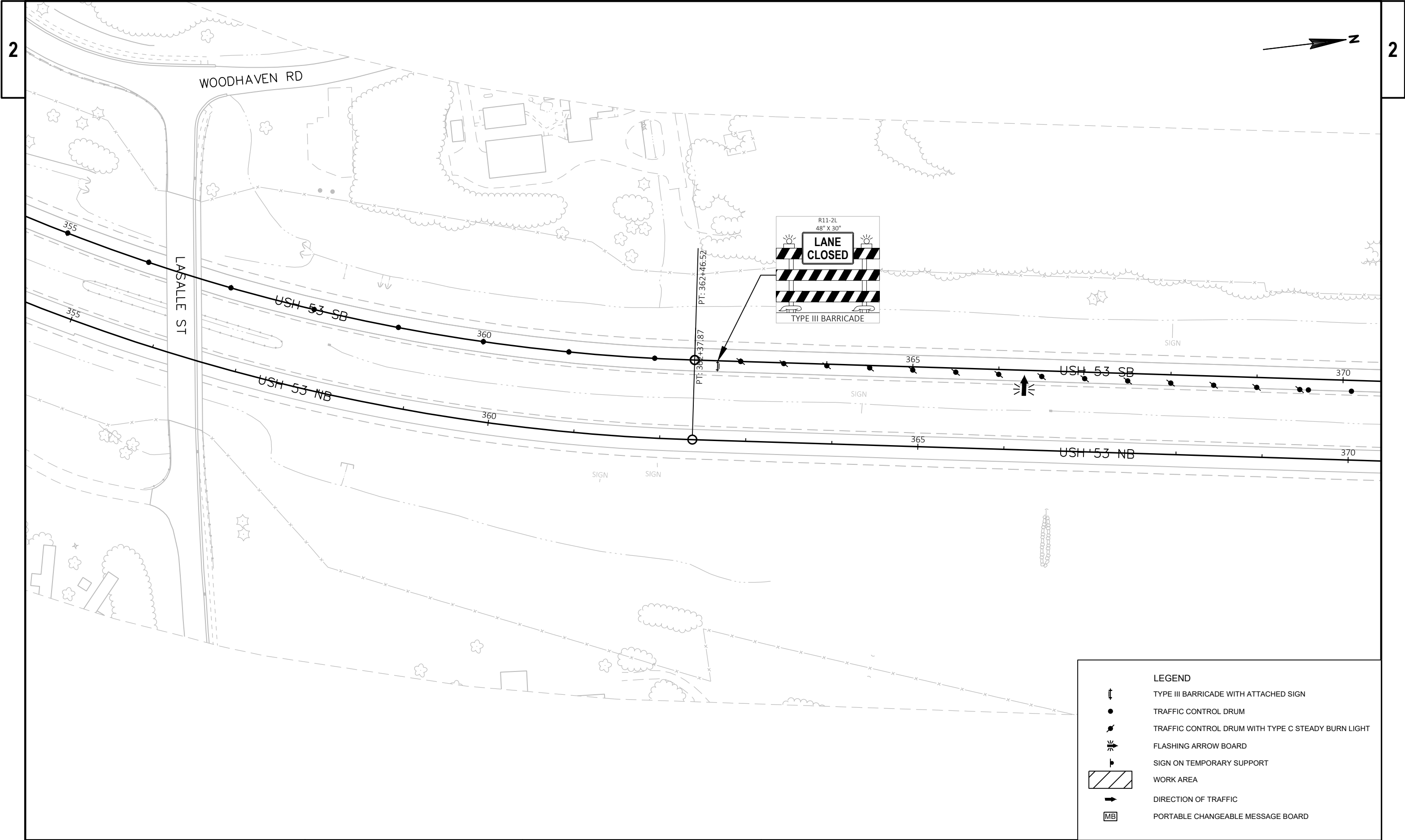


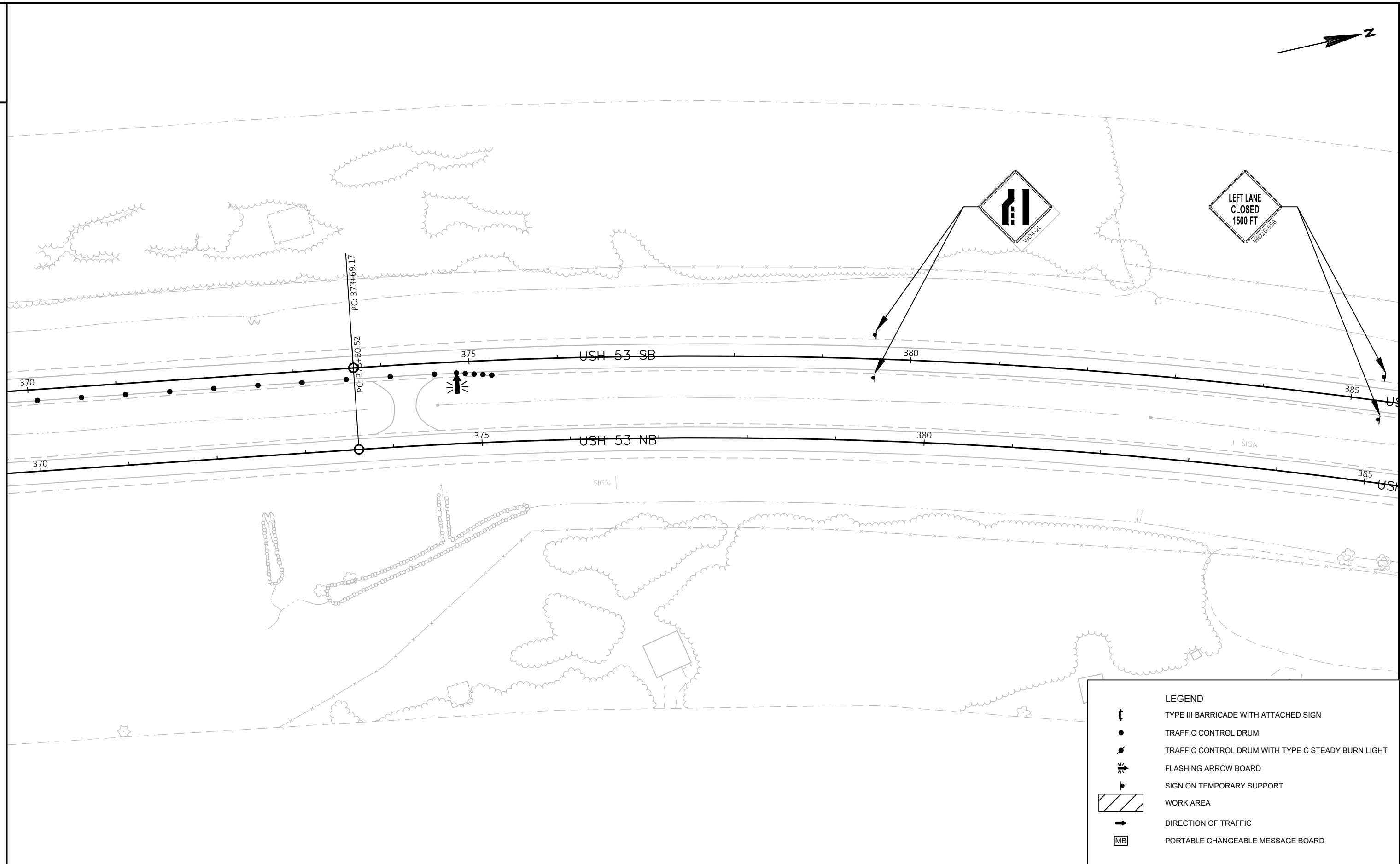




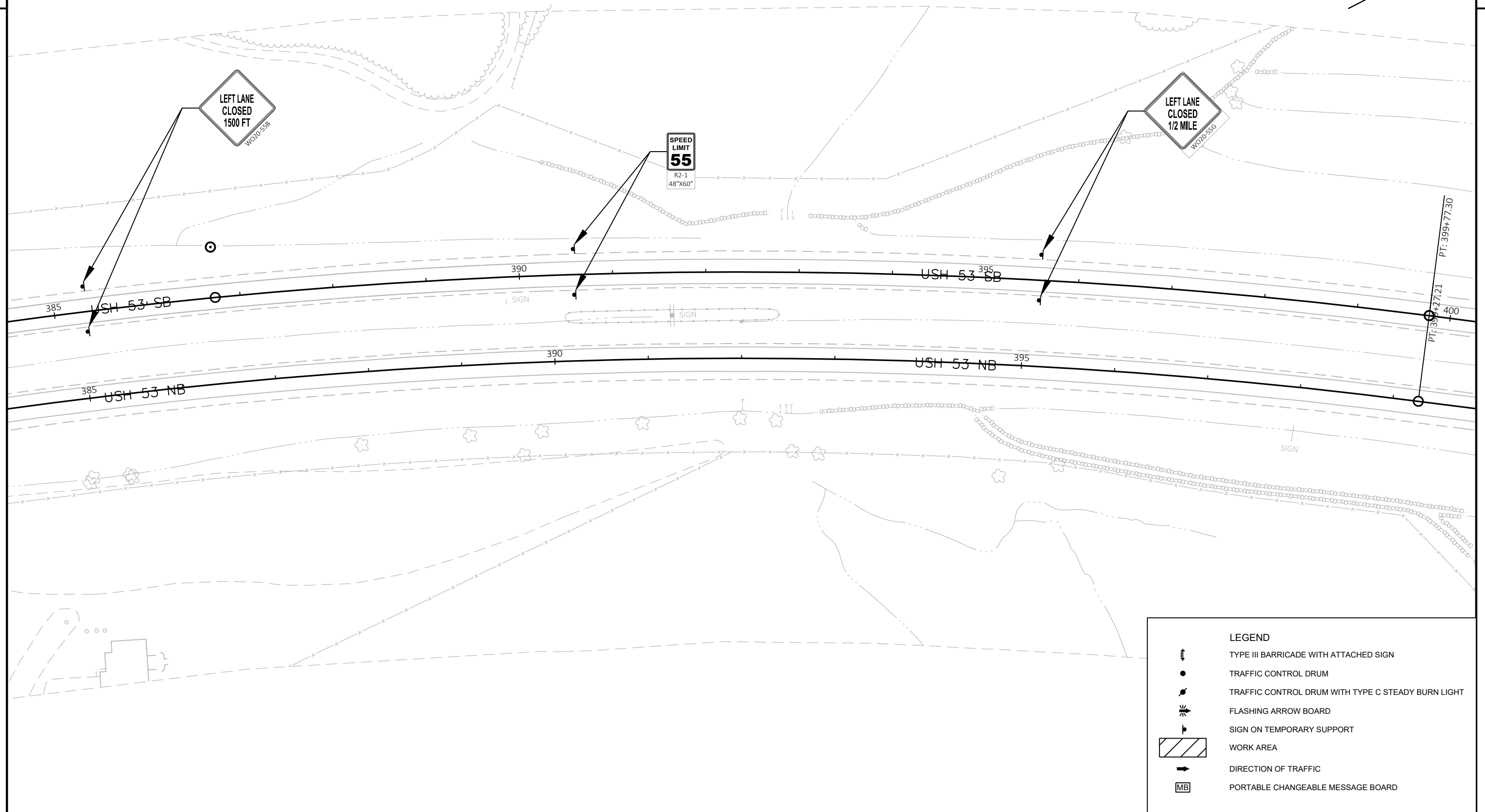




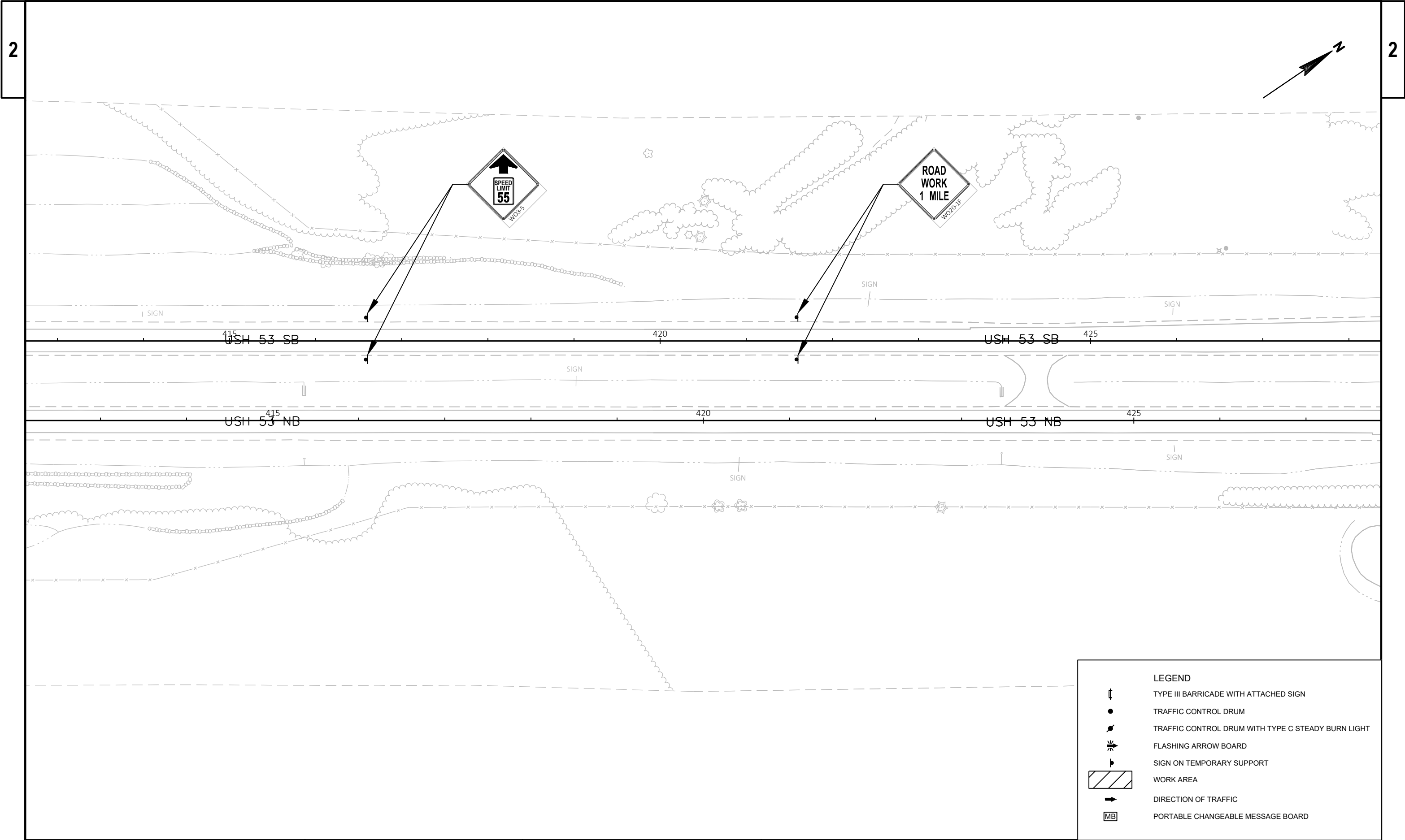




PROJECT NO: 1190-02-64	HWY: USH 53	COUNTY: EAU CLAIRE	TRAFFIC CONTROL - STAGE 2 - B-18-168 EAU CLAIRE RIVER	SHEET	E
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PROJECT NO: 1190-02-64	HWY: USH 53	COUNTY: EAU CLAIRE	TRAFFIC CONTROL - STAGE 2 - B-18-168 EAU CLAIRE RIVER	SHEET	E
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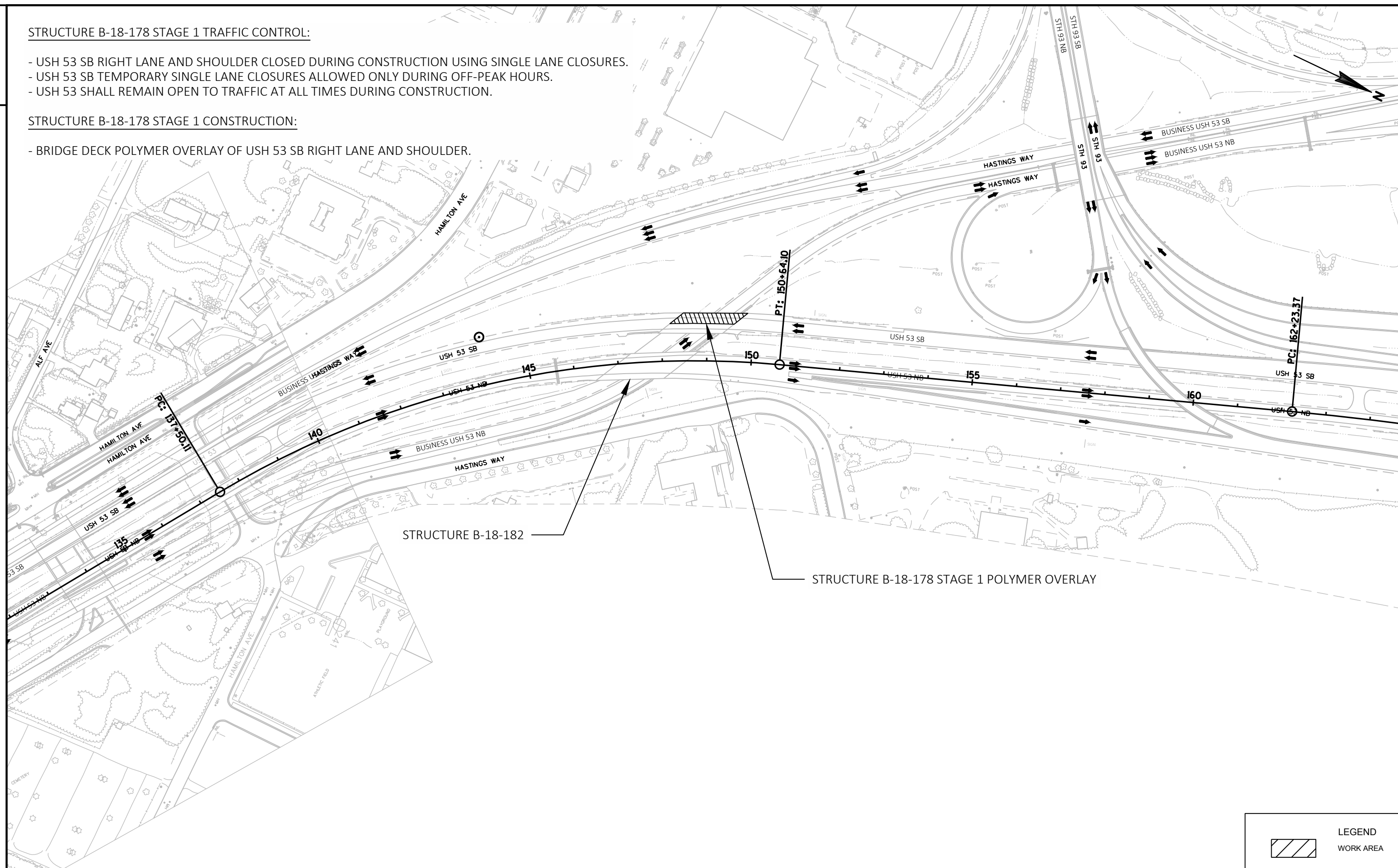


STRUCTURE B-18-178 STAGE 1 TRAFFIC CONTROL:

- USH 53 SB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-178 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB RIGHT LANE AND SHOULDER.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 1 OVERVIEW - B-18-178

SHEET

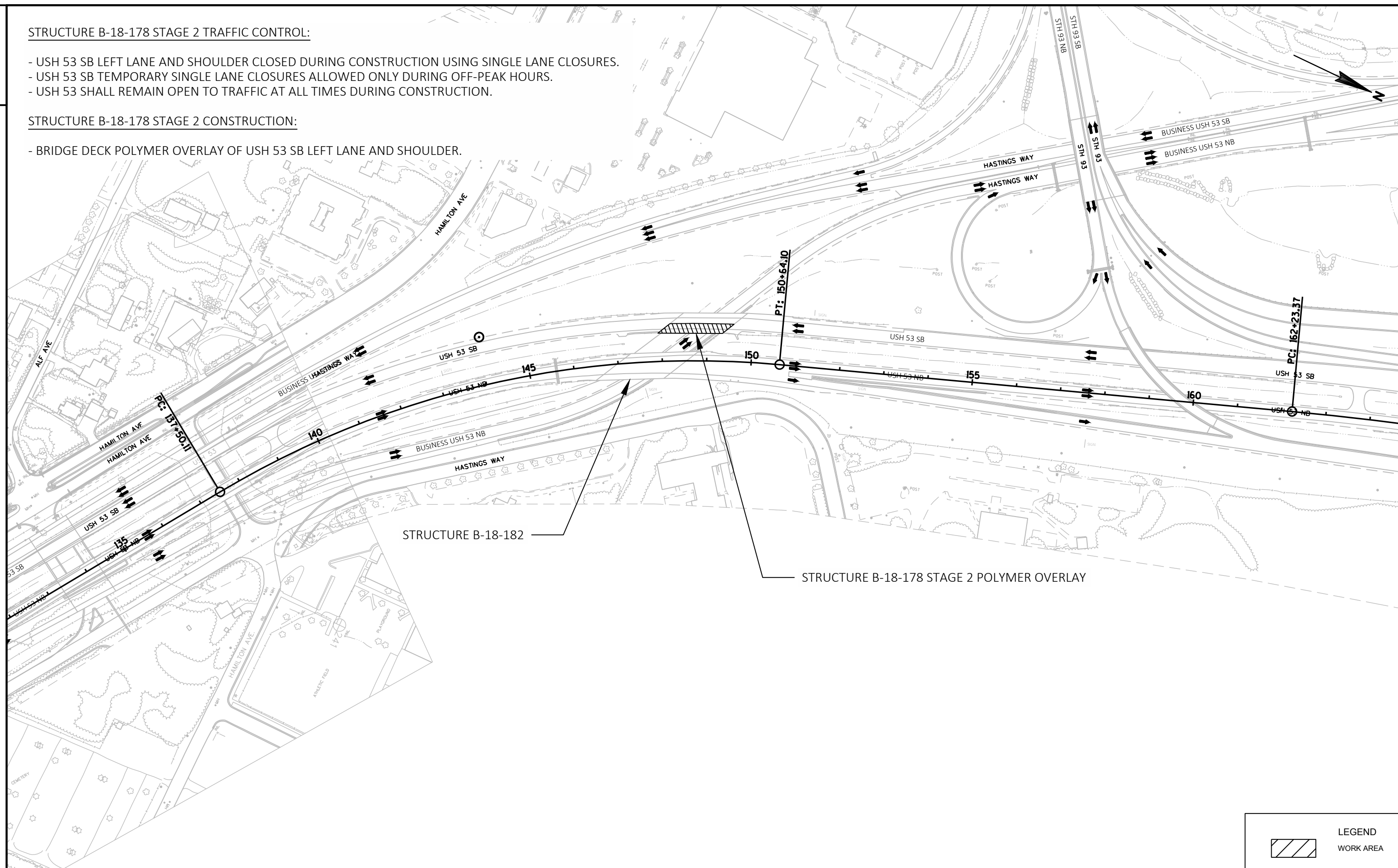
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STRUCTURE B-18-178 STAGE 2 TRAFFIC CONTROL:

- USH 53 SB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-178 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB LEFT LANE AND SHOULDER.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 2 OVERVIEW - B-18-178

SHEET

E



2

STRUCTURE B-18-184 STAGE 1 TRAFFIC CONTROL:

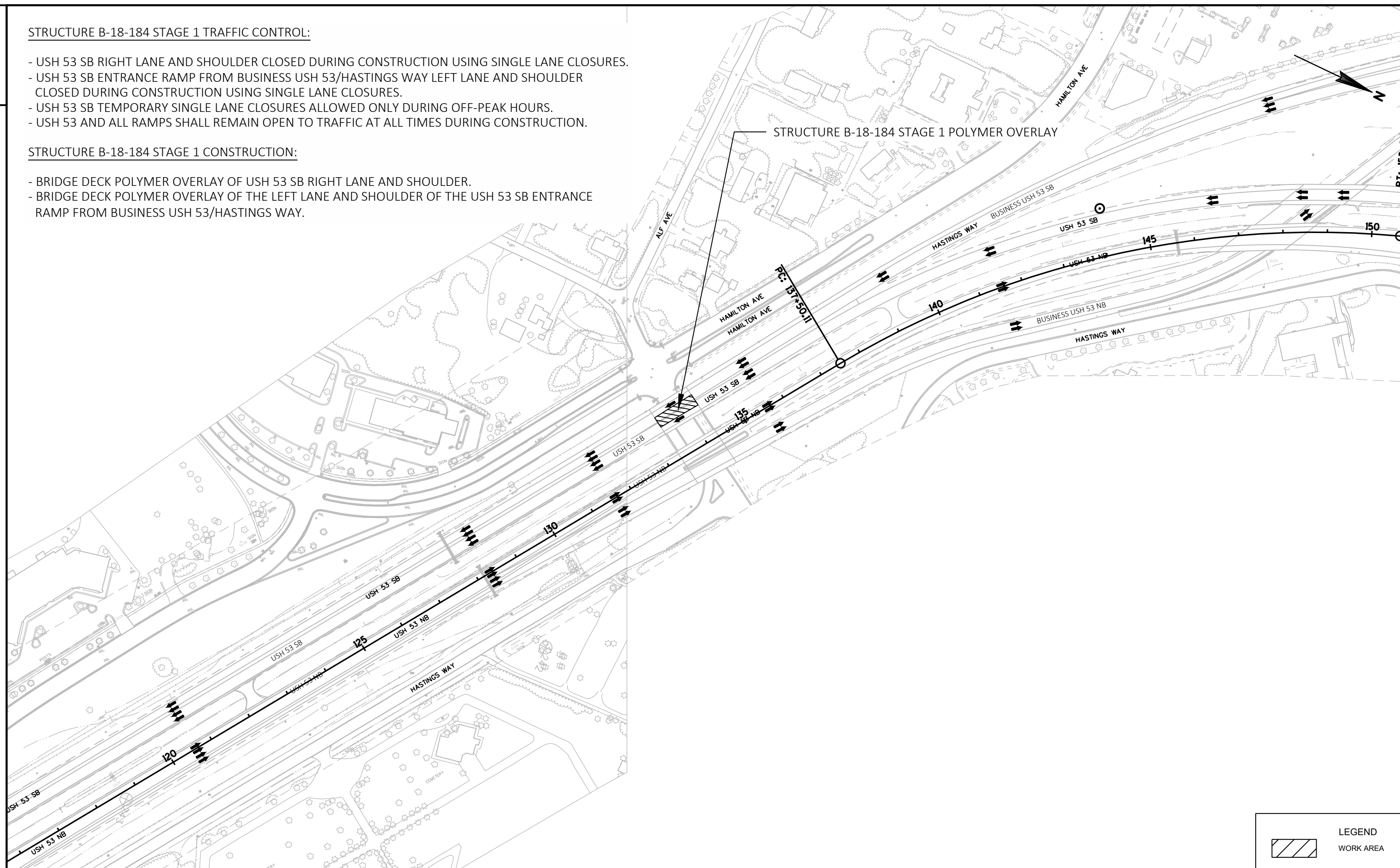
- USH 53 SB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-184 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB RIGHT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE LEFT LANE AND SHOULDER OF THE USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY.

- STRUCTURE B-18-184 STAGE 1 POLYMER OVERLAY

2



PROJECT NO:	1190-02-64
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HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 1 OVERVIEW - B-18-184

SHEET

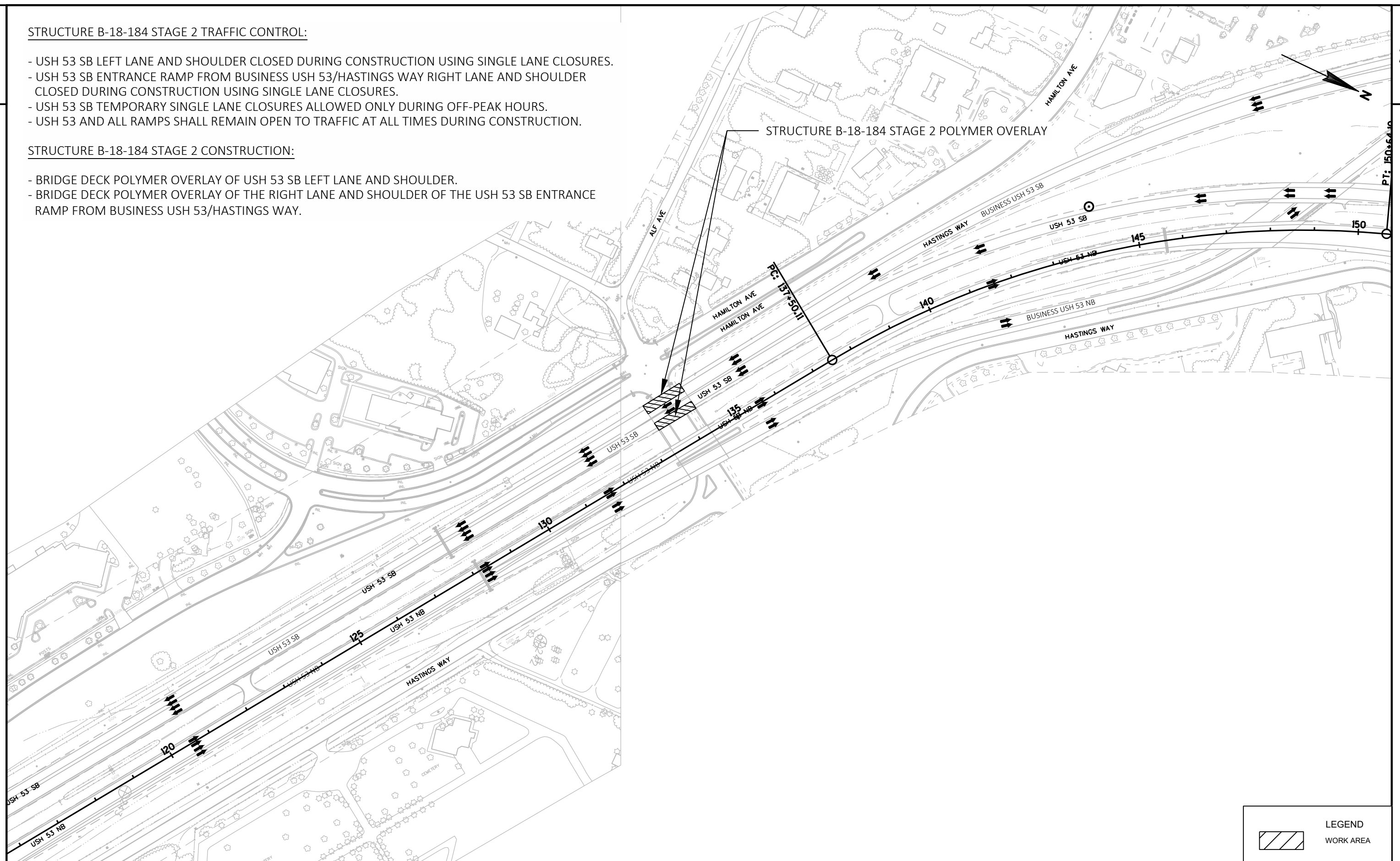
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STRUCTURE B-18-184 STAGE 2 TRAFFIC CONTROL:

- USH 53 SB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMP SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-184 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB LEFT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE RIGHT LANE AND SHOULDER OF THE USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY.

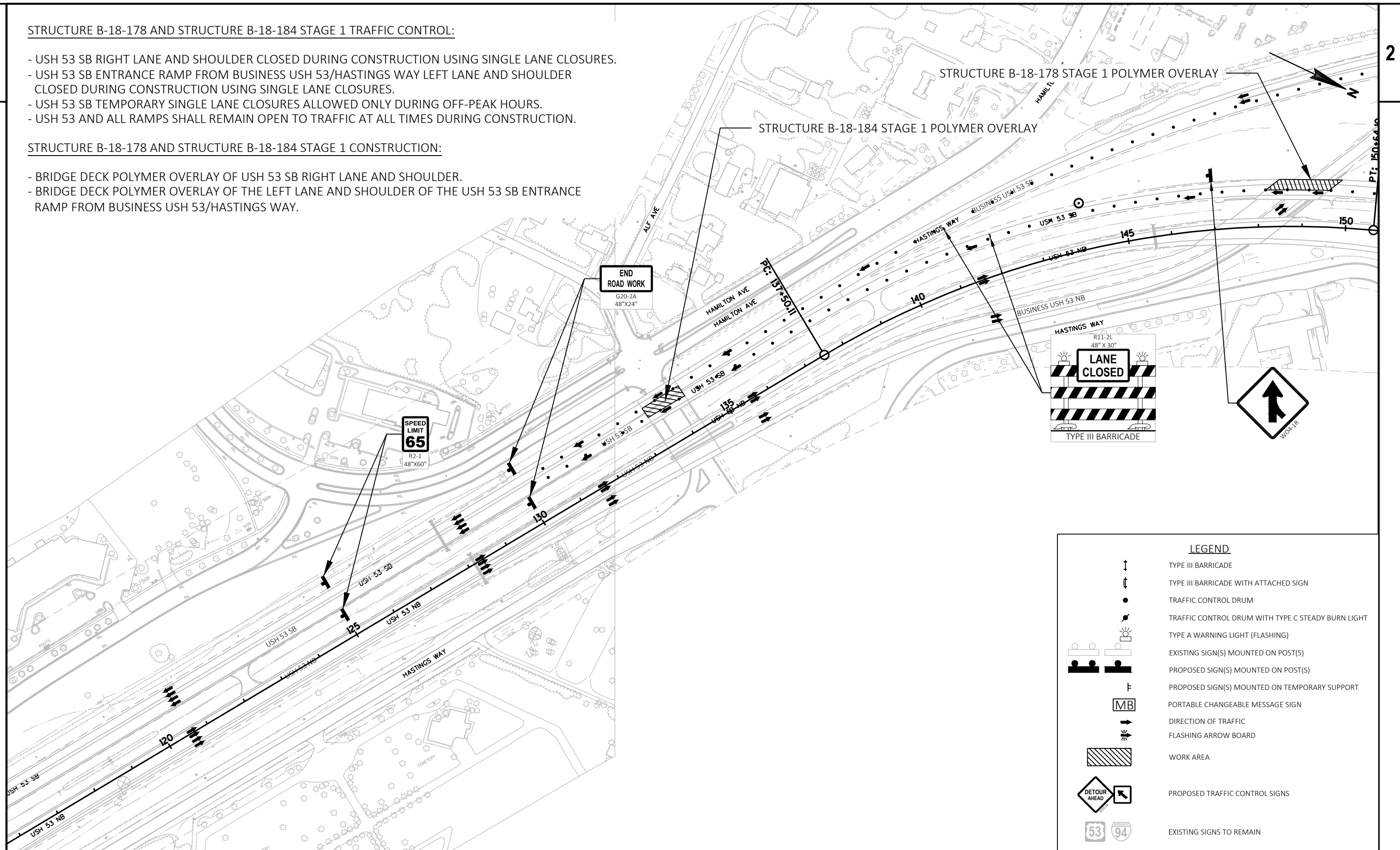


STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 1 TRAFFIC CONTROL:

- USH 53 SB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMP SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB RIGHT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE LEFT LANE AND SHOULDER OF THE USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY.



PROJECT NO: 1190-02-64

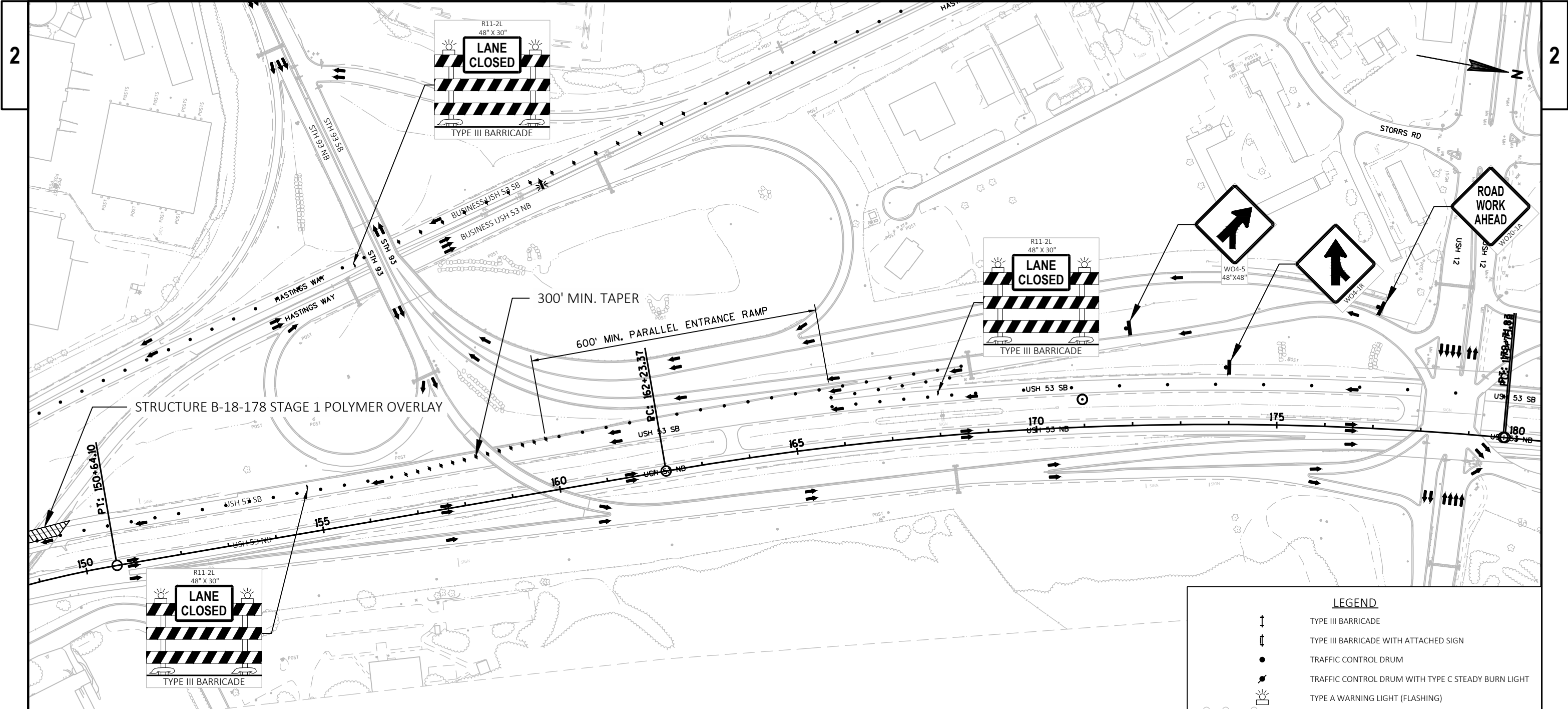
HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - B-18-178 AND B-18-184 - STAGE 1 PLAN

SHEET

E



**STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 1 TRAFFIC CONTROL:**

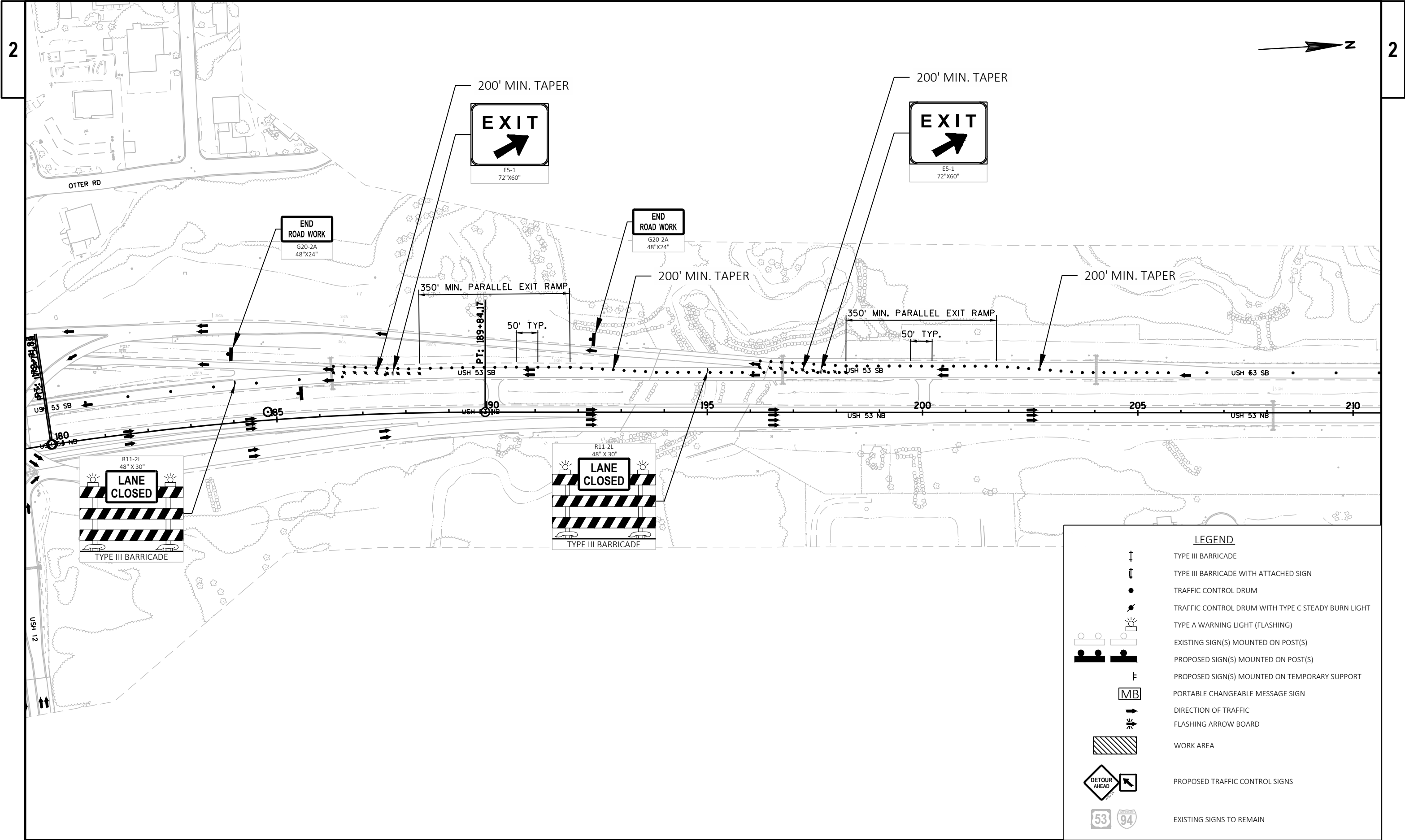
- USH 53 SB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
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- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

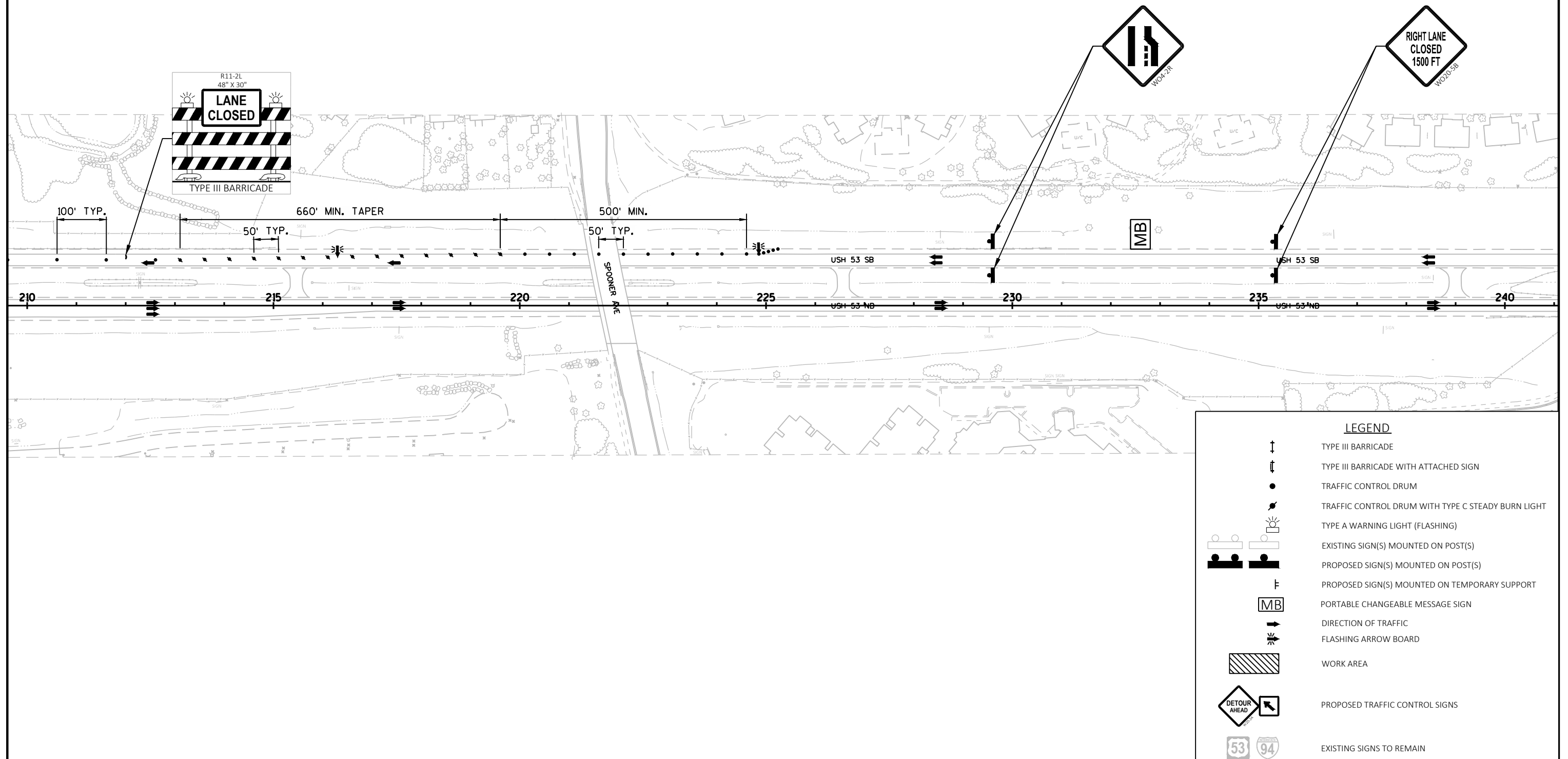
**STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 1 CONSTRUCTION:**

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB RIGHT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE LEFT LANE AND SHOULDER OF THE USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY.

**LEGEND**

	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	TYPE A WARNING LIGHT (FLASHING)
	EXISTING SIGN(S) MOUNTED ON POST(S)
	PROPOSED SIGN(S) MOUNTED ON POST(S)
	PROPOSED SIGN(S) MOUNTED ON TEMPORARY SUPPORT
	PORTABLE CHANGEABLE MESSAGE SIGN
	DIRECTION OF TRAFFIC
	FLASHING ARROW BOARD
	WORK AREA
	PROPOSED TRAFFIC CONTROL SIGNS
	EXISTING SIGNS TO REMAIN





PROJECT NO: 1190-02-64

HWY: USH 53

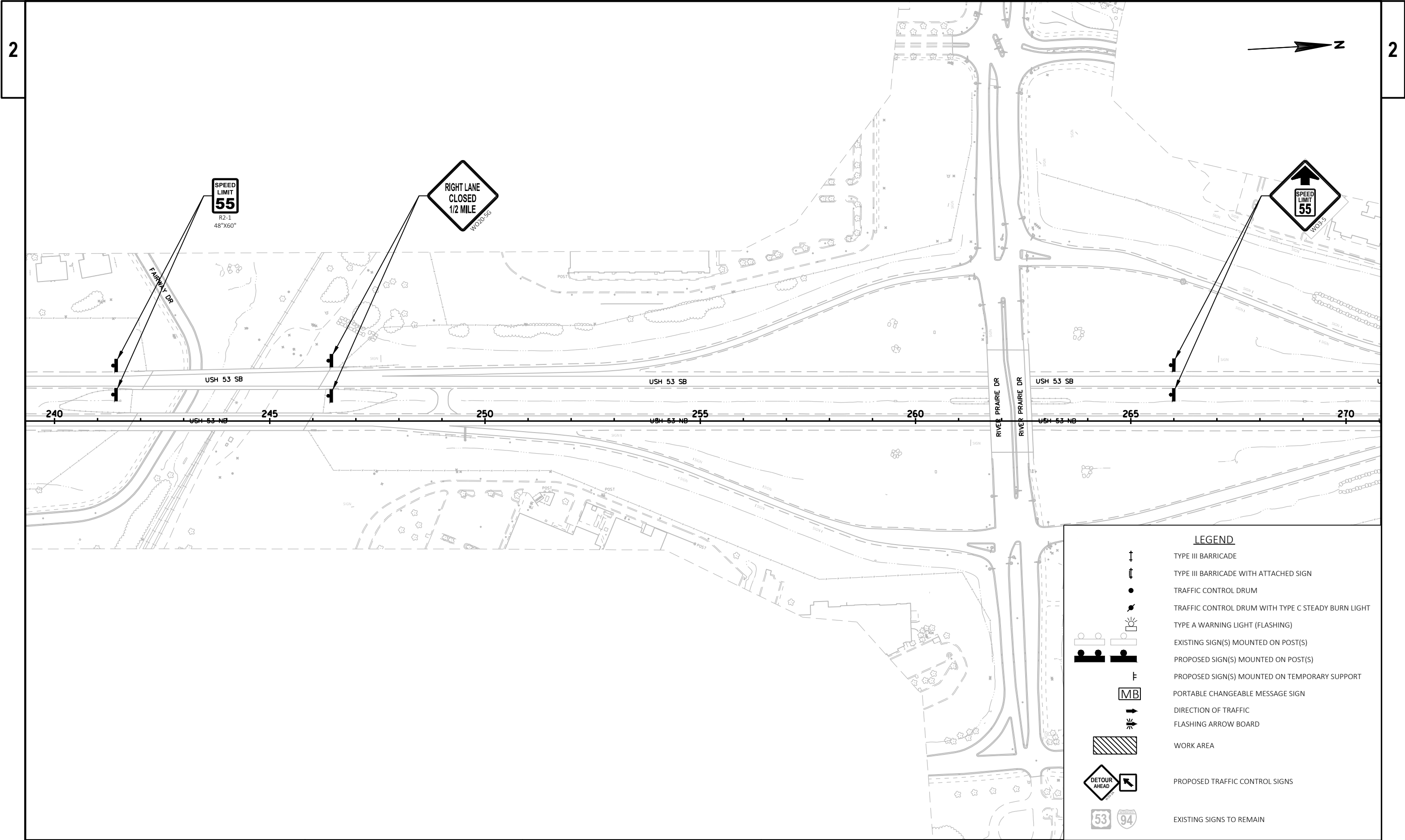
COUNTY: EAU CLAIRE

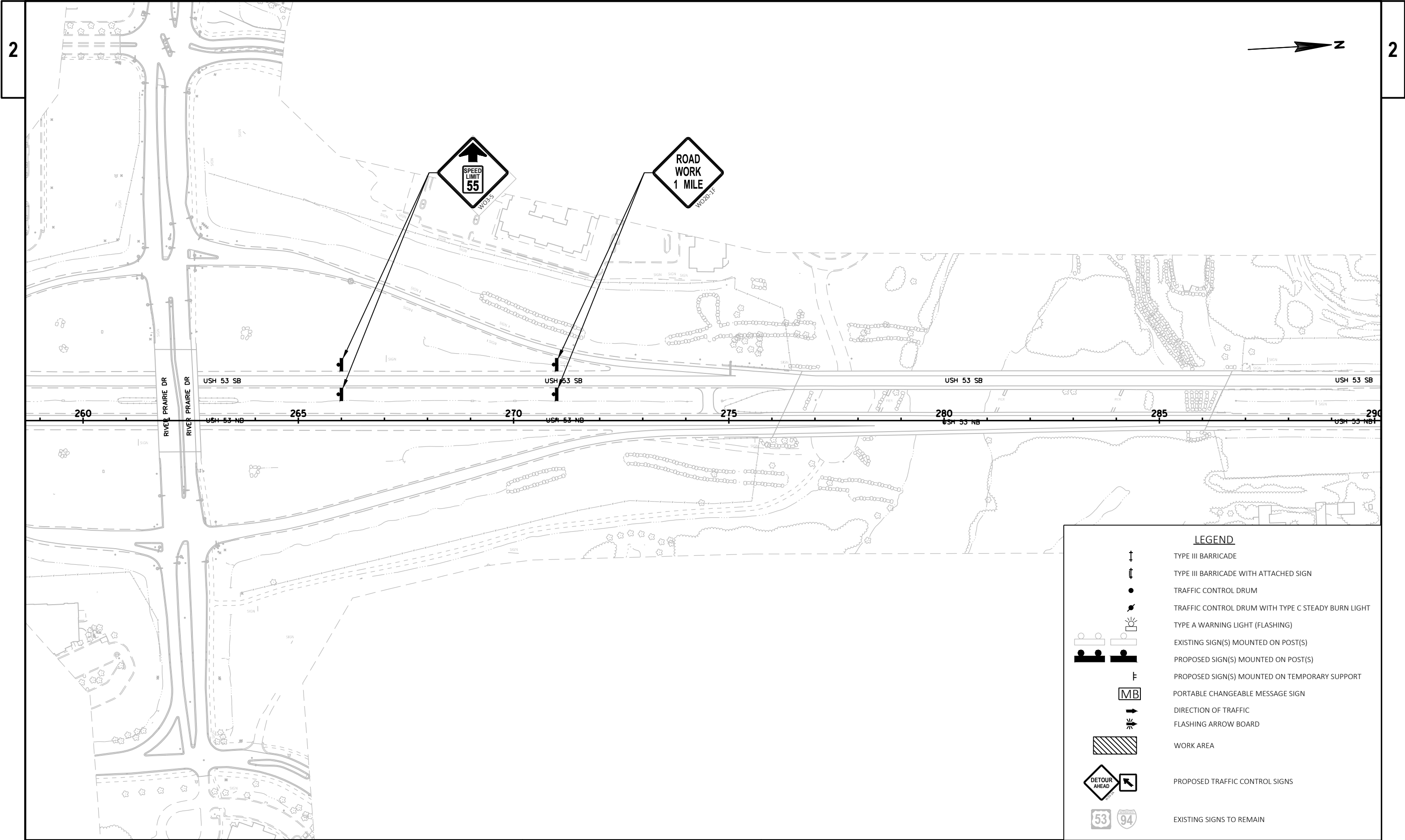
TRAFFIC CONTROL - B-18-178 AND B-18-184 - STAGE 1 PLAN

SHEET

E





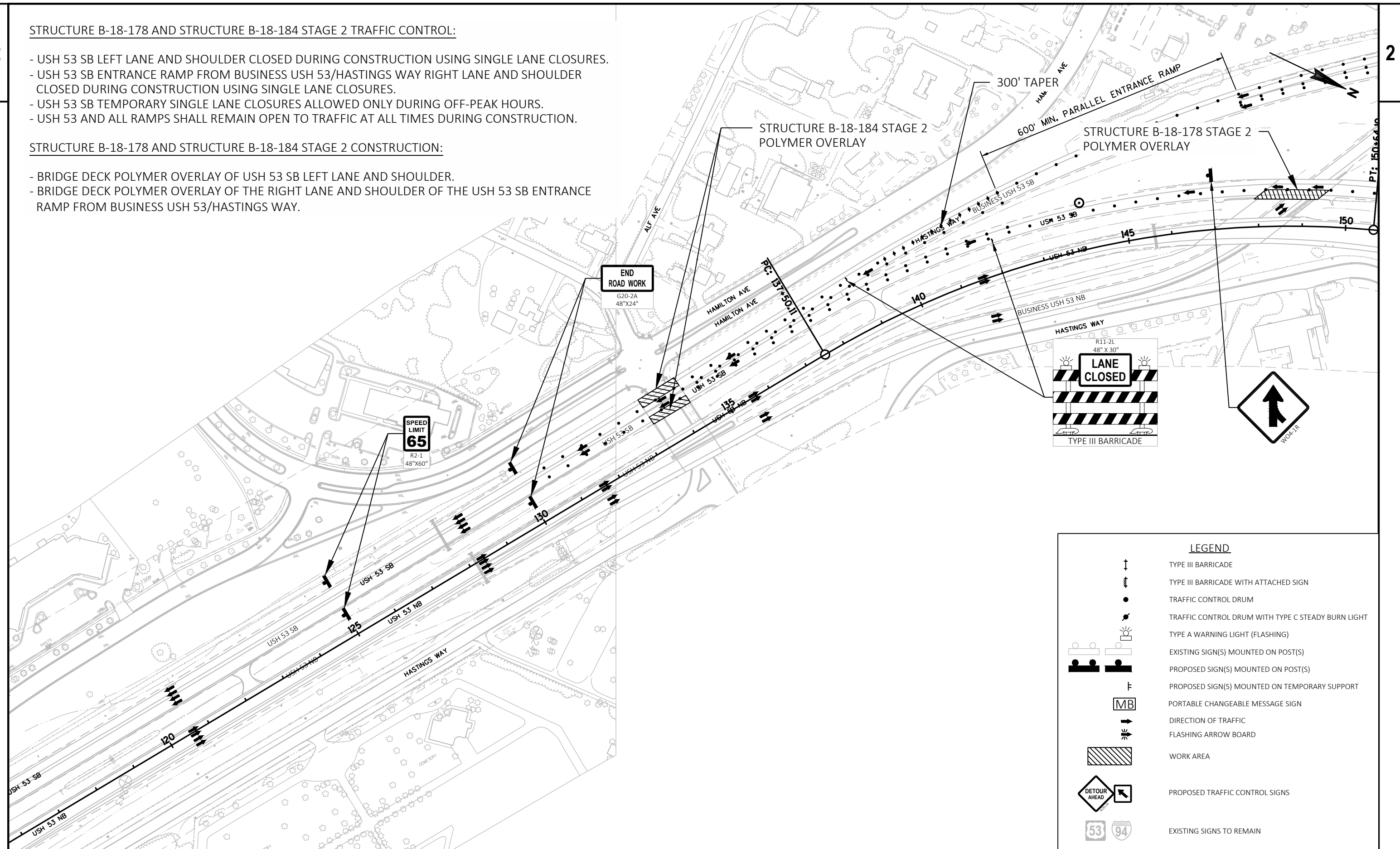


STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 2 TRAFFIC CONTROL:

- USH 53 SB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPs SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB LEFT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE RIGHT LANE AND SHOULDER OF THE USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY.



PROJECT NO: 1190-02-64

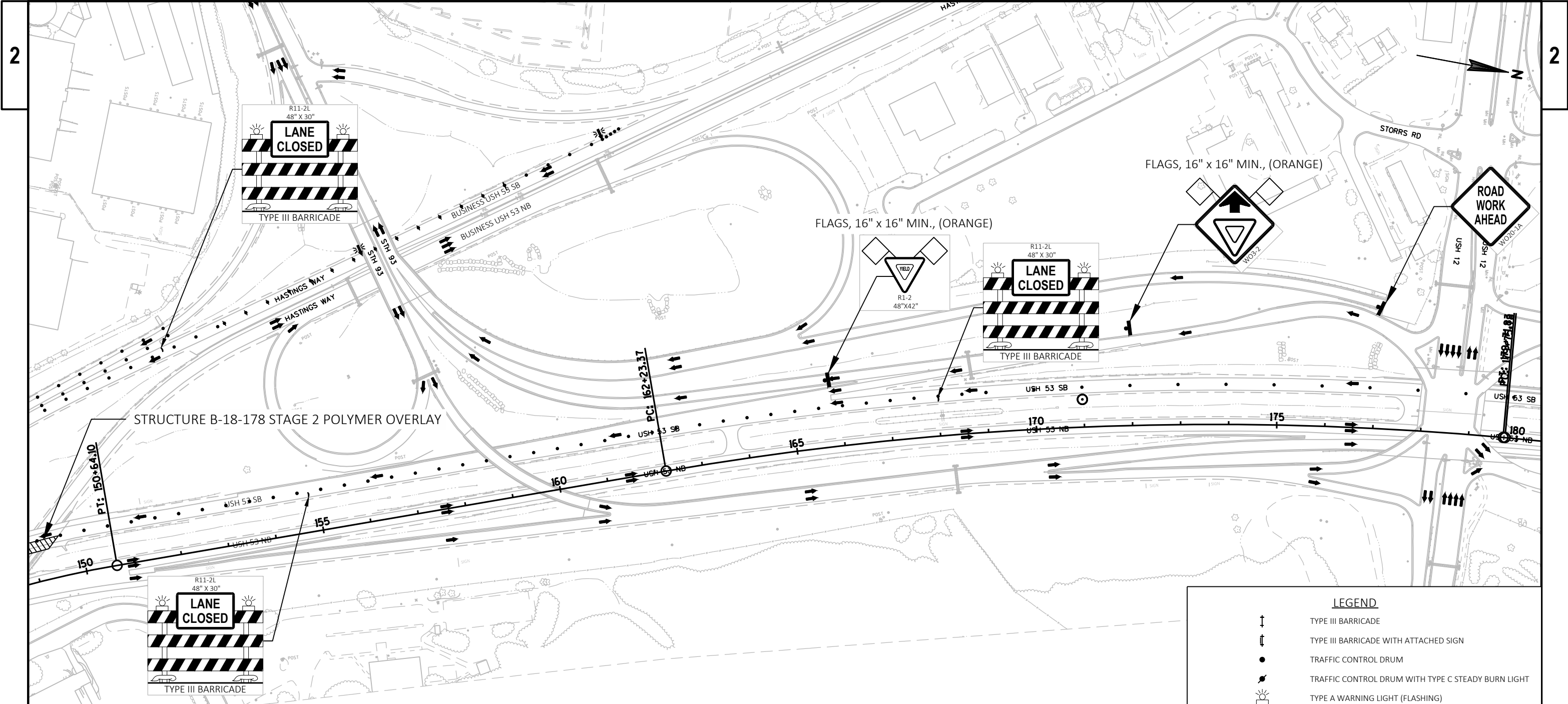
HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - B-18-178 AND B-18-184 - STAGE 2 PLAN

SHEET

E



**STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 2 TRAFFIC CONTROL:**

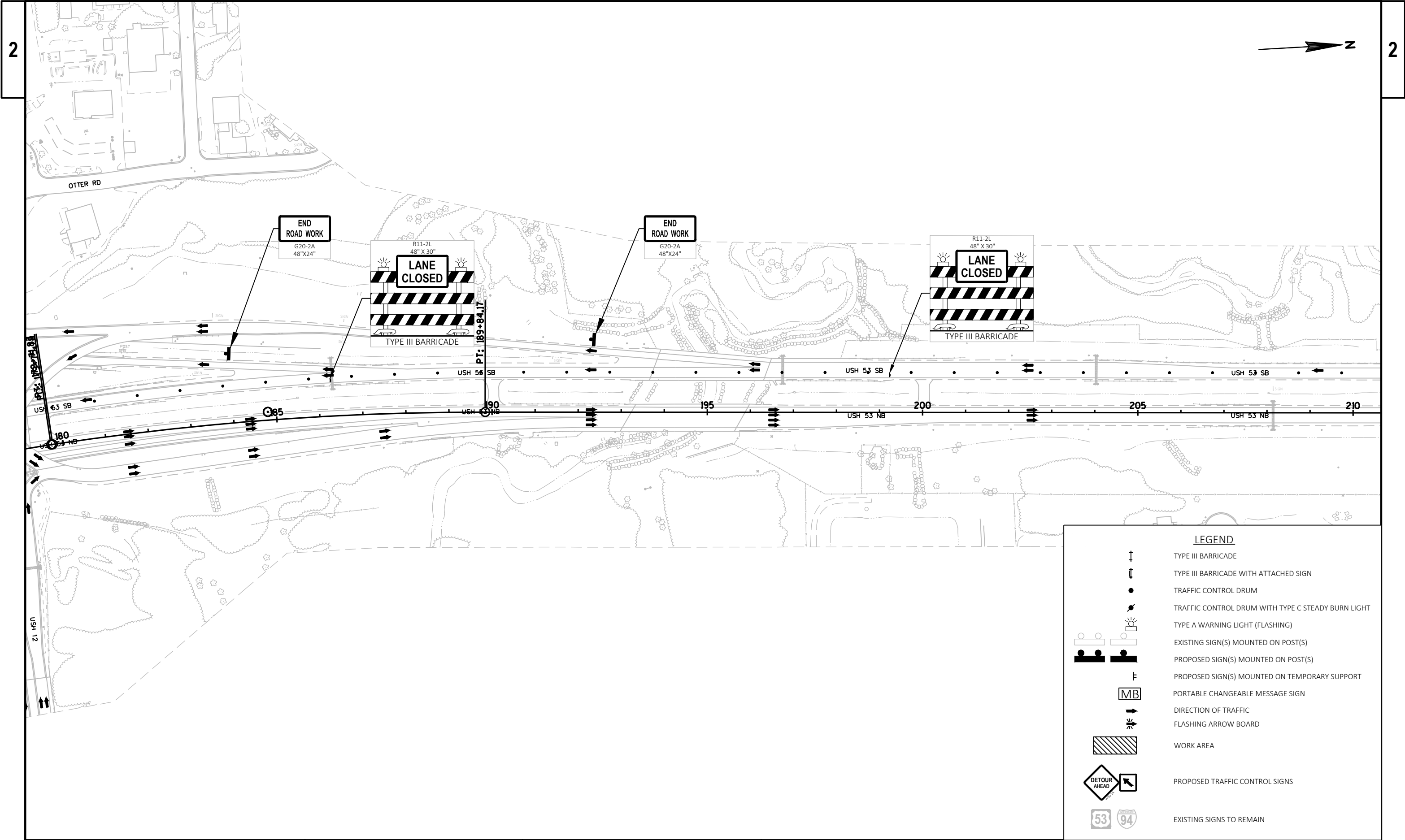
- USH 53 SB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 SB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

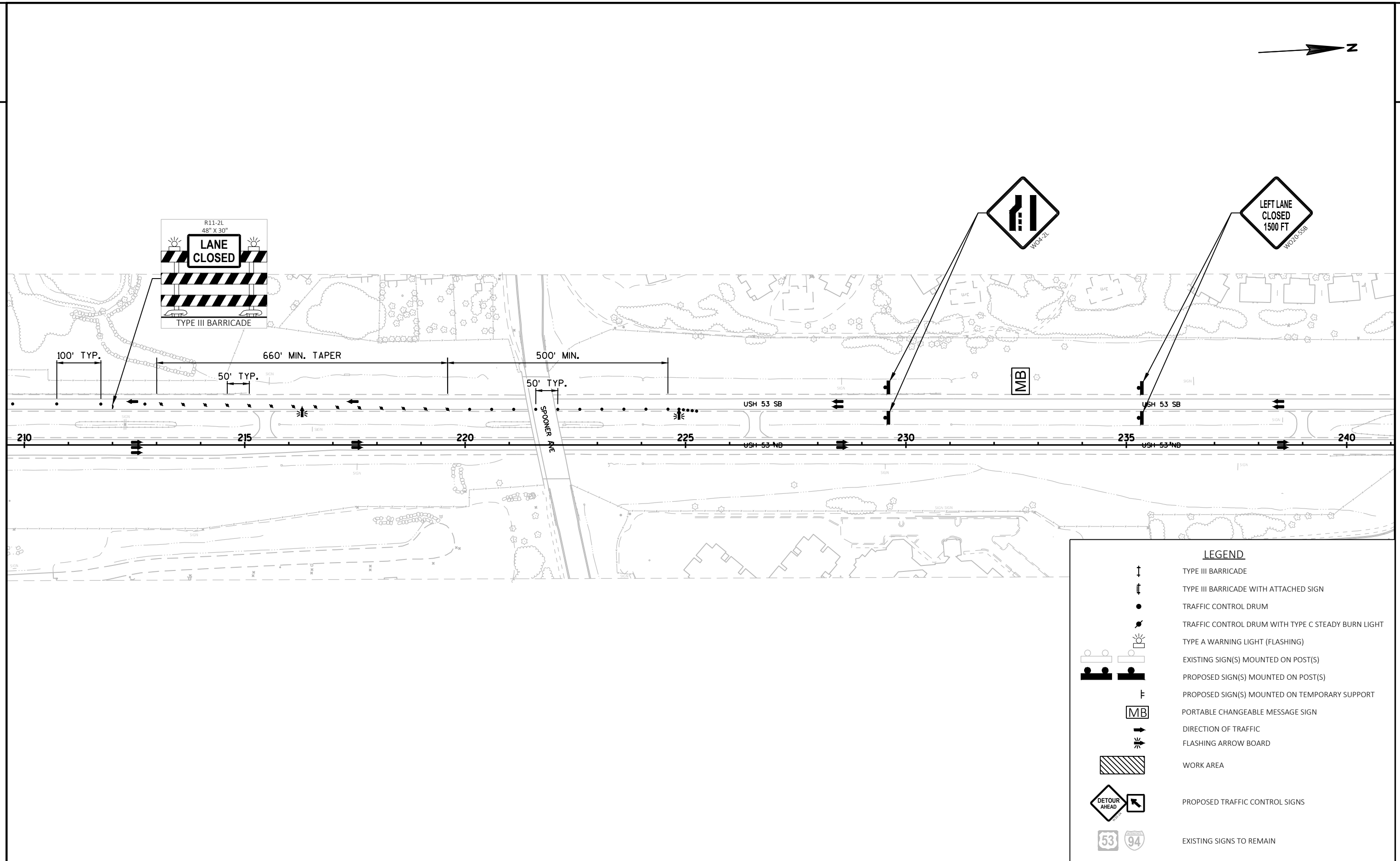
**STRUCTURE B-18-178 AND STRUCTURE B-18-184 STAGE 2 CONSTRUCTION:**

- BRIDGE DECK POLYMER OVERLAY OF USH 53 SB LEFT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE RIGHT LANE AND SHOULDER OF THE USH 53 SB ENTRANCE RAMP FROM BUSINESS USH 53/HASTINGS WAY.

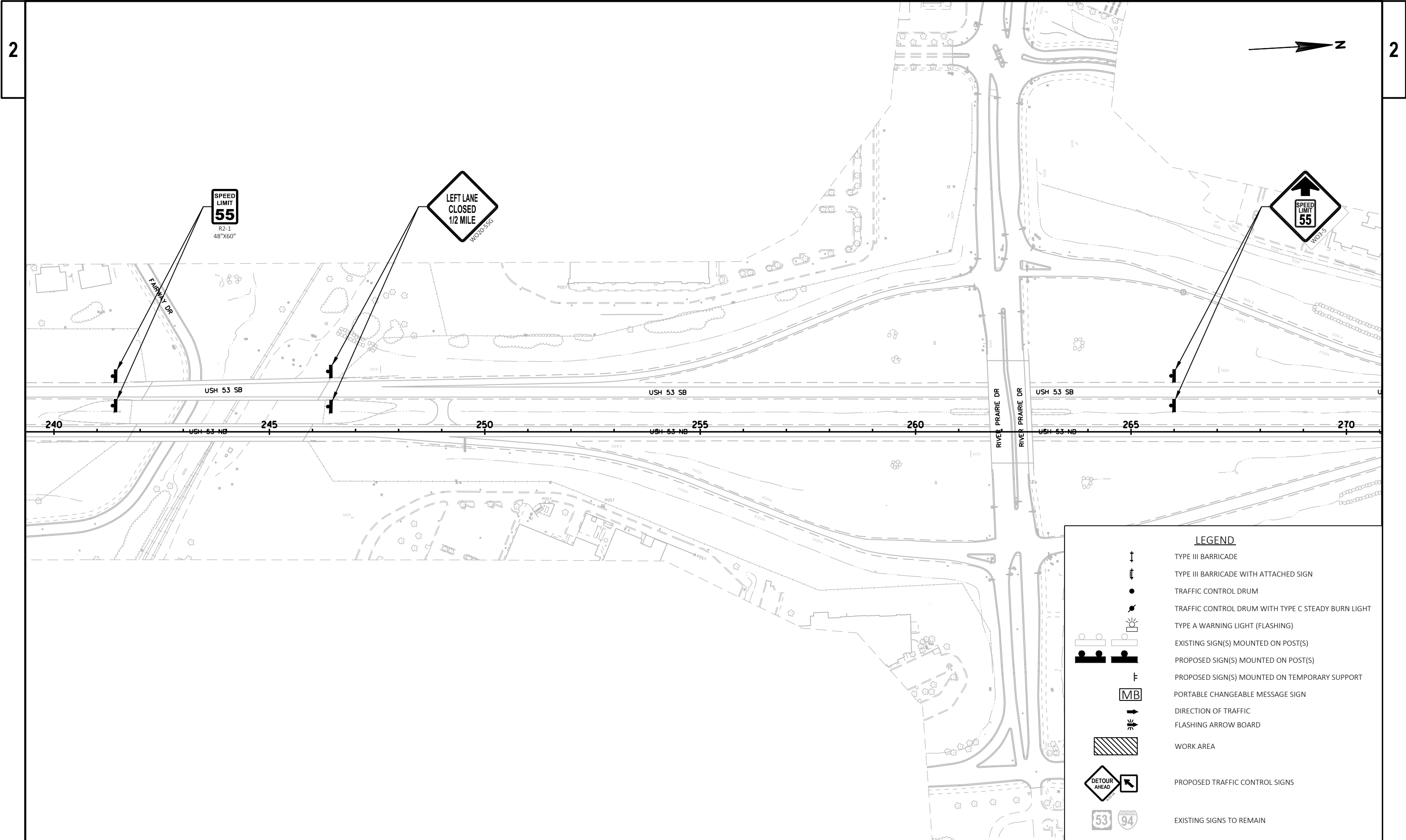
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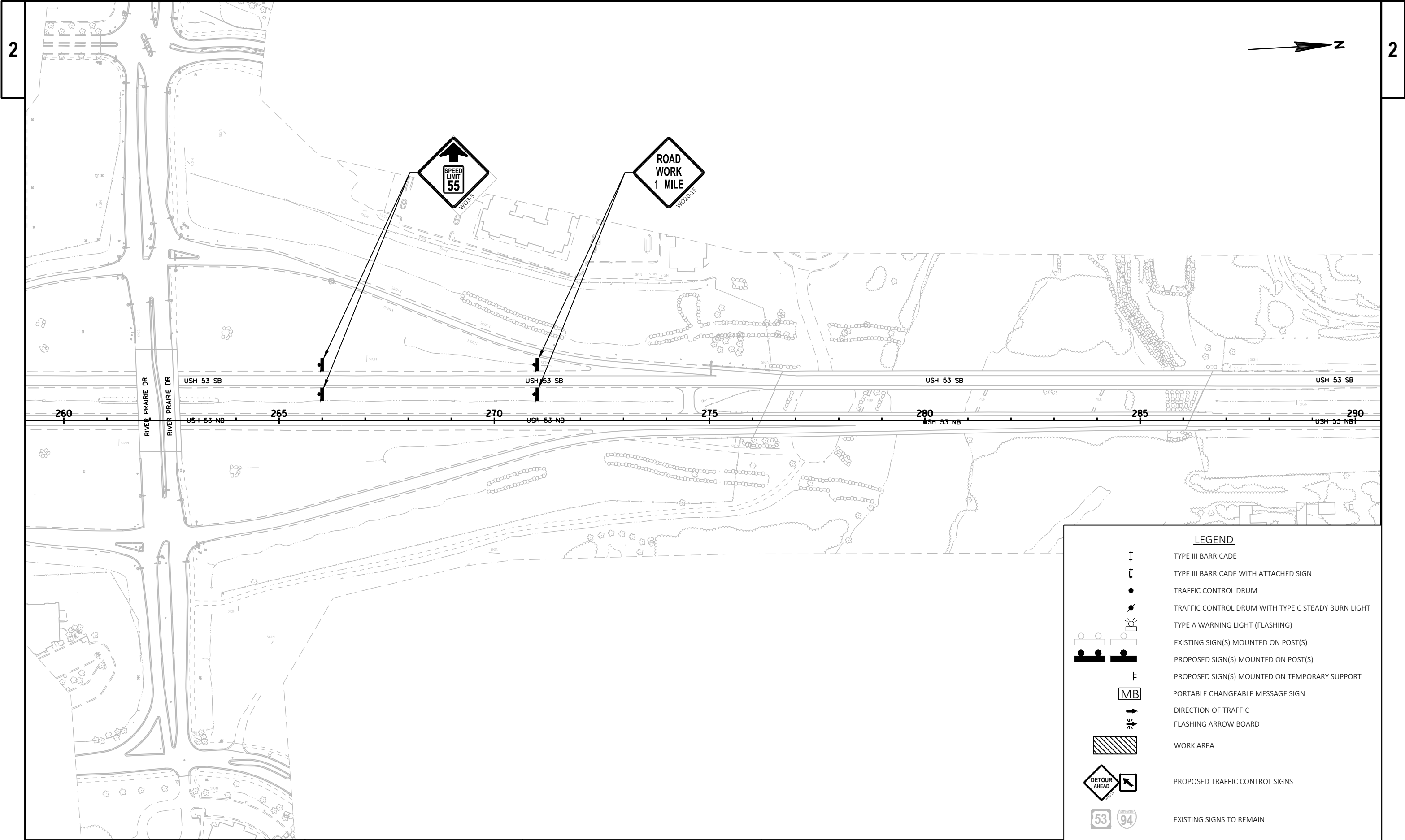
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	TYPE A WARNING LIGHT (FLASHING)
	EXISTING SIGN(S) MOUNTED ON POST(S)
	PROPOSED SIGN(S) MOUNTED ON POST(S)
	PROPOSED SIGN(S) MOUNTED ON TEMPORARY SUPPORT
	PORTABLE CHANGEABLE MESSAGE SIGN
	DIRECTION OF TRAFFIC
	FLASHING ARROW BOARD
	WORK AREA
	PROPOSED TRAFFIC CONTROL SIGNS
	EXISTING SIGNS TO REMAIN











**LEGEND**

TYPE III BARRICADE

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

TYPE A WARNING LIGHT (FLASHING)

EXISTING SIGN(S) MOUNTED ON POST(S)

PROPOSED SIGN(S) MOUNTED ON POST(S)

PROPOSED SIGN(S) MOUNTED ON TEMPORARY SUPPORT

PORTABLE CHANGEABLE MESSAGE SIGN

DIRECTION OF TRAFFIC

FLASHING ARROW BOARD

WORK AREA

PROPOSED TRAFFIC CONTROL SIGNS

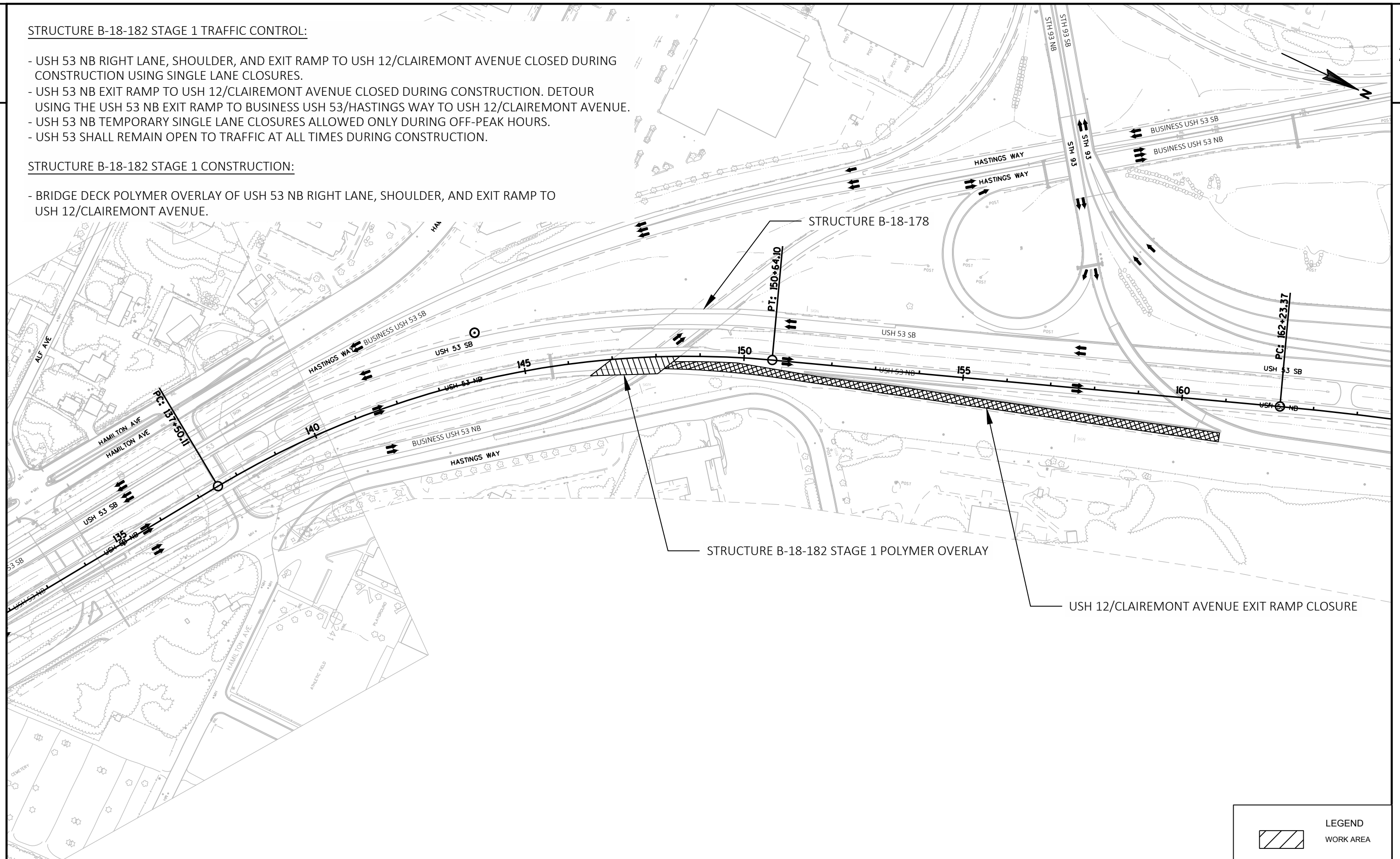
EXISTING SIGNS TO REMAIN

STRUCTURE B-18-182 STAGE 1 TRAFFIC CONTROL:

- USH 53 NB RIGHT LANE, SHOULDER, AND EXIT RAMP TO USH 12/CLAIREMONT AVENUE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB EXIT RAMP TO USH 12/CLAIREMONT AVENUE CLOSED DURING CONSTRUCTION. DETOUR USING THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY TO USH 12/CLAIREMONT AVENUE.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-182 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB RIGHT LANE, SHOULDER, AND EXIT RAMP TO USH 12/CLAIREMONT AVENUE.

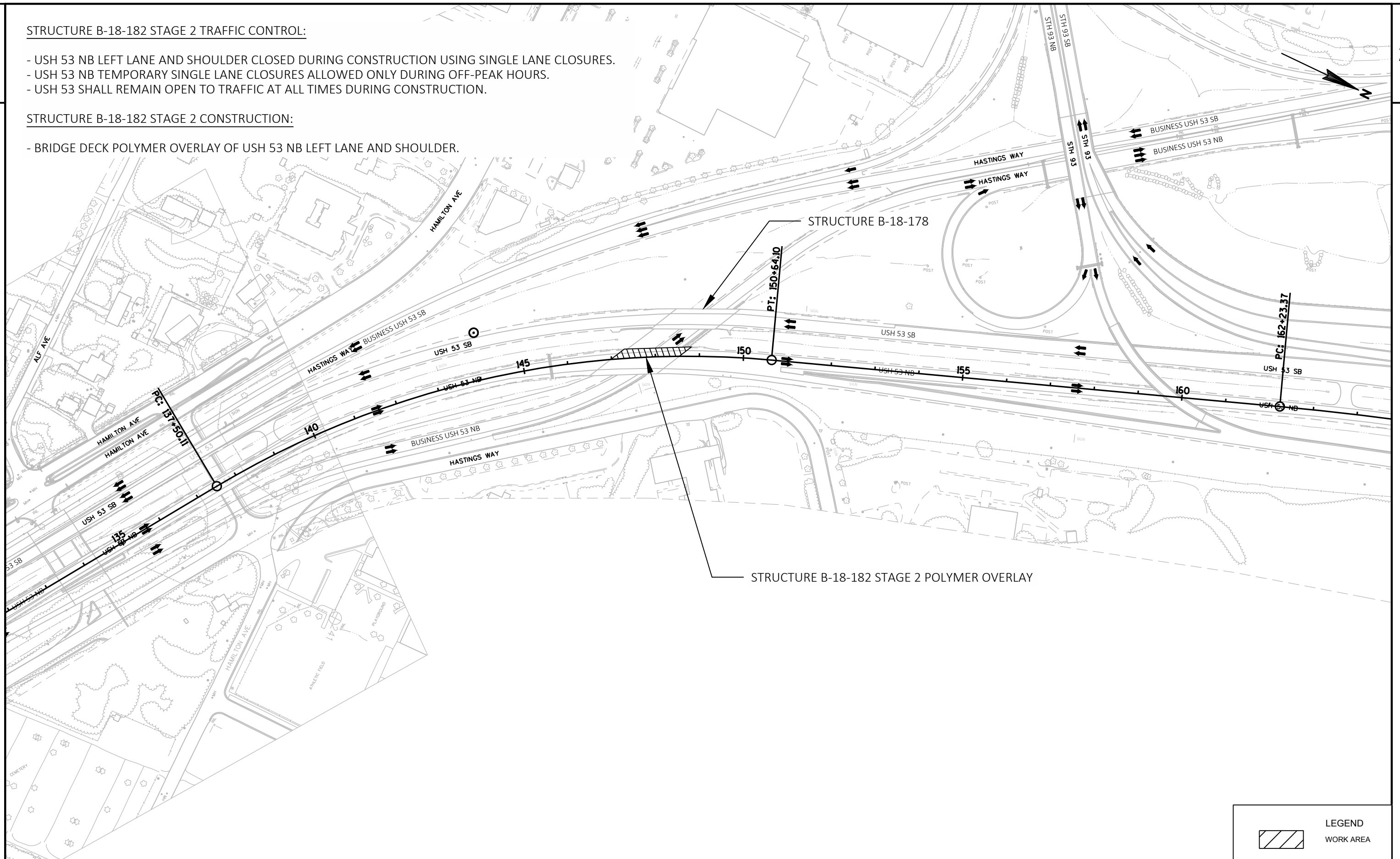


STRUCTURE B-18-182 STAGE 2 TRAFFIC CONTROL:

- USH 53 NB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-182 STAGE 2 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB LEFT LANE AND SHOULDER.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 2 OVERVIEW - B-18-182

SHEET

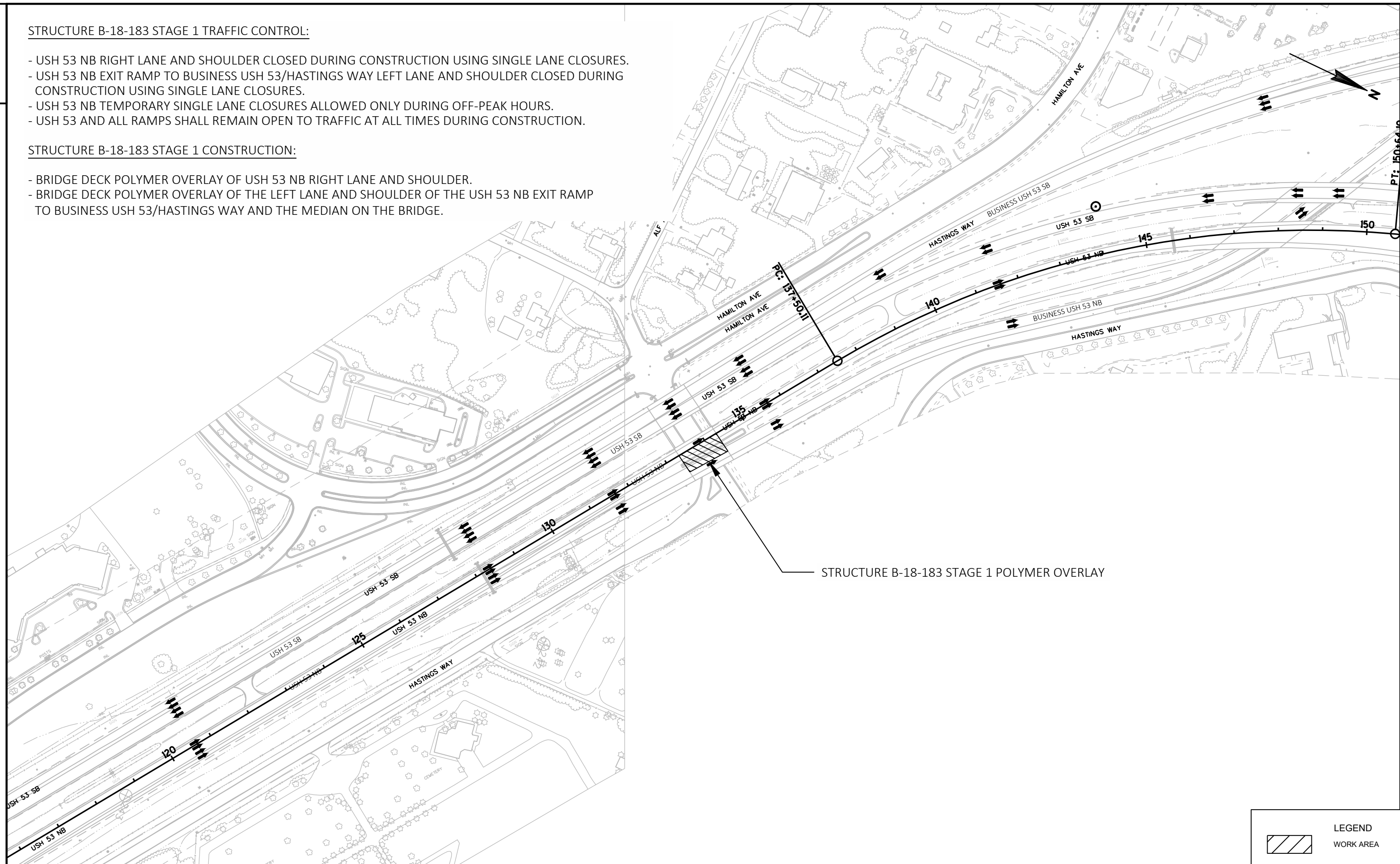
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STRUCTURE B-18-183 STAGE 1 TRAFFIC CONTROL:

- USH 53 NB RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-183 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB RIGHT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE LEFT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY AND THE MEDIAN ON THE BRIDGE.

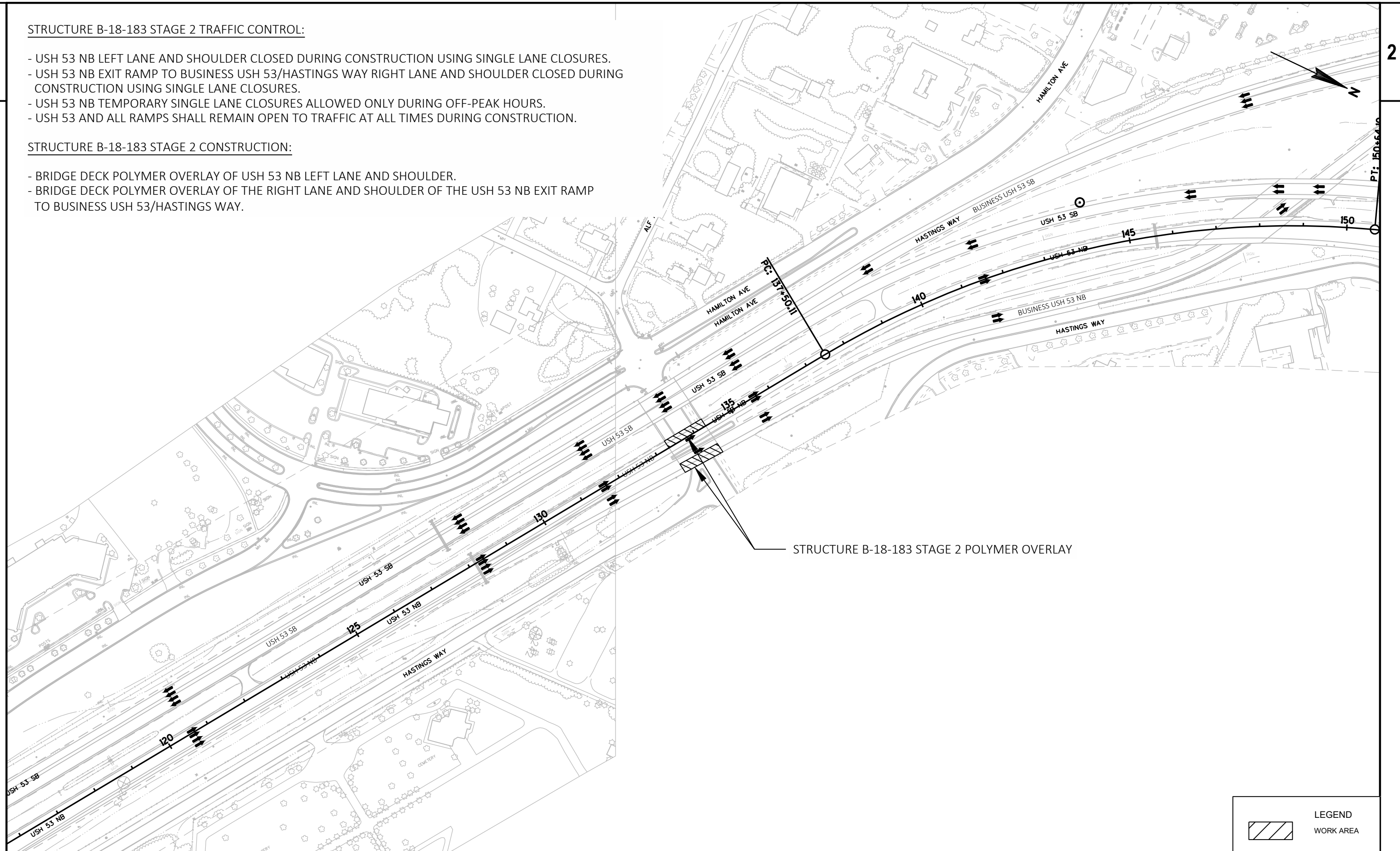


STRUCTURE B-18-183 STAGE 2 TRAFFIC CONTROL:

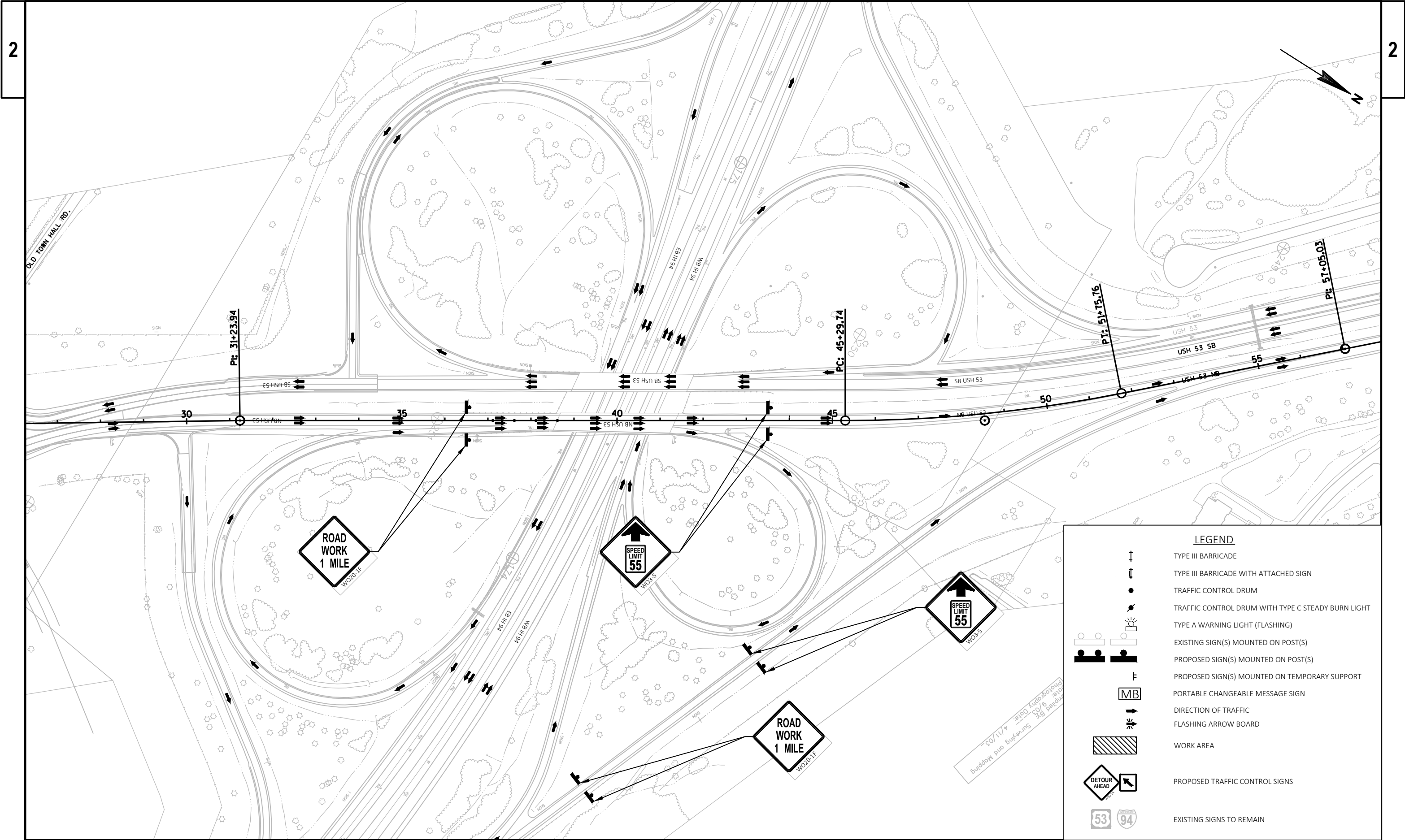
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- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

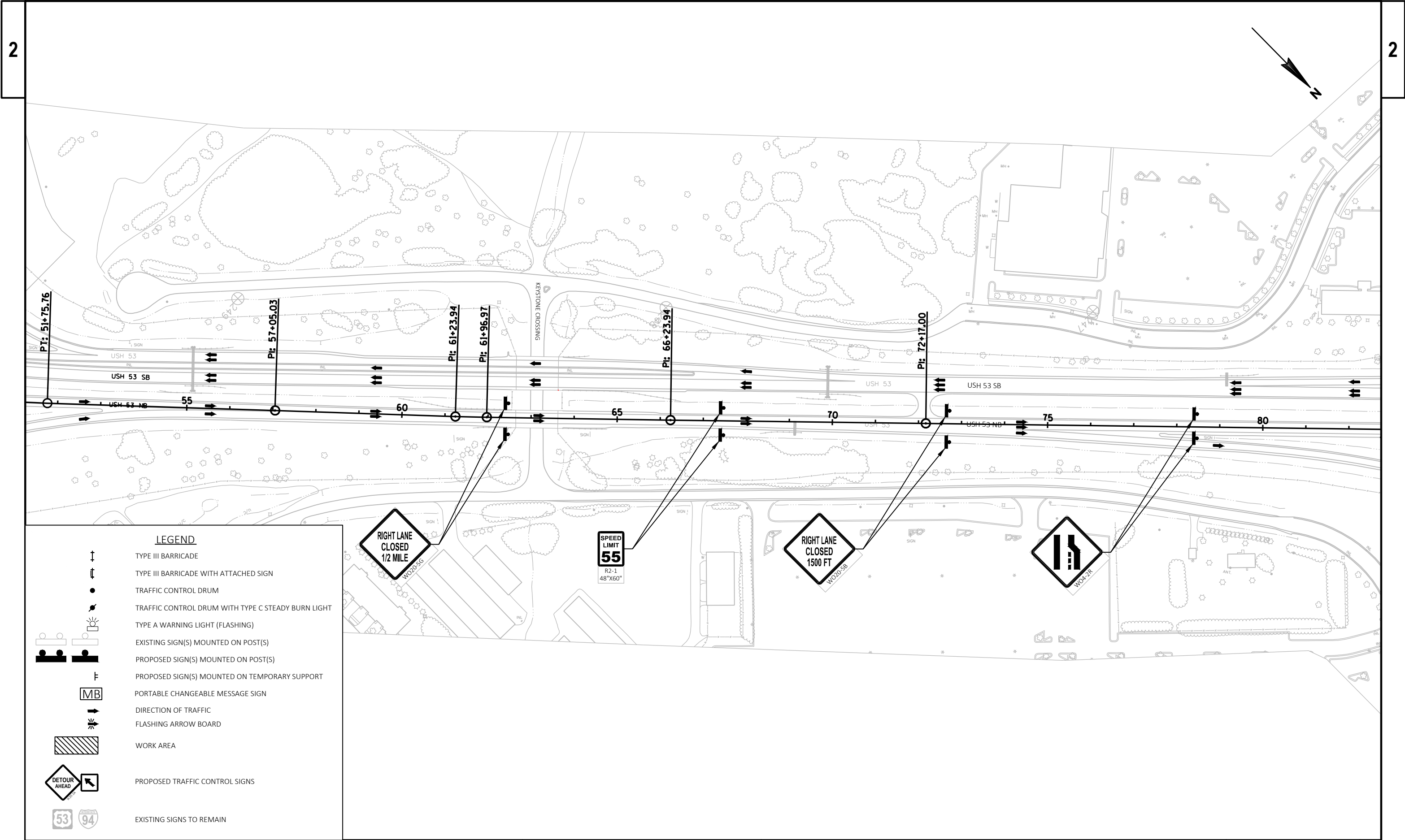
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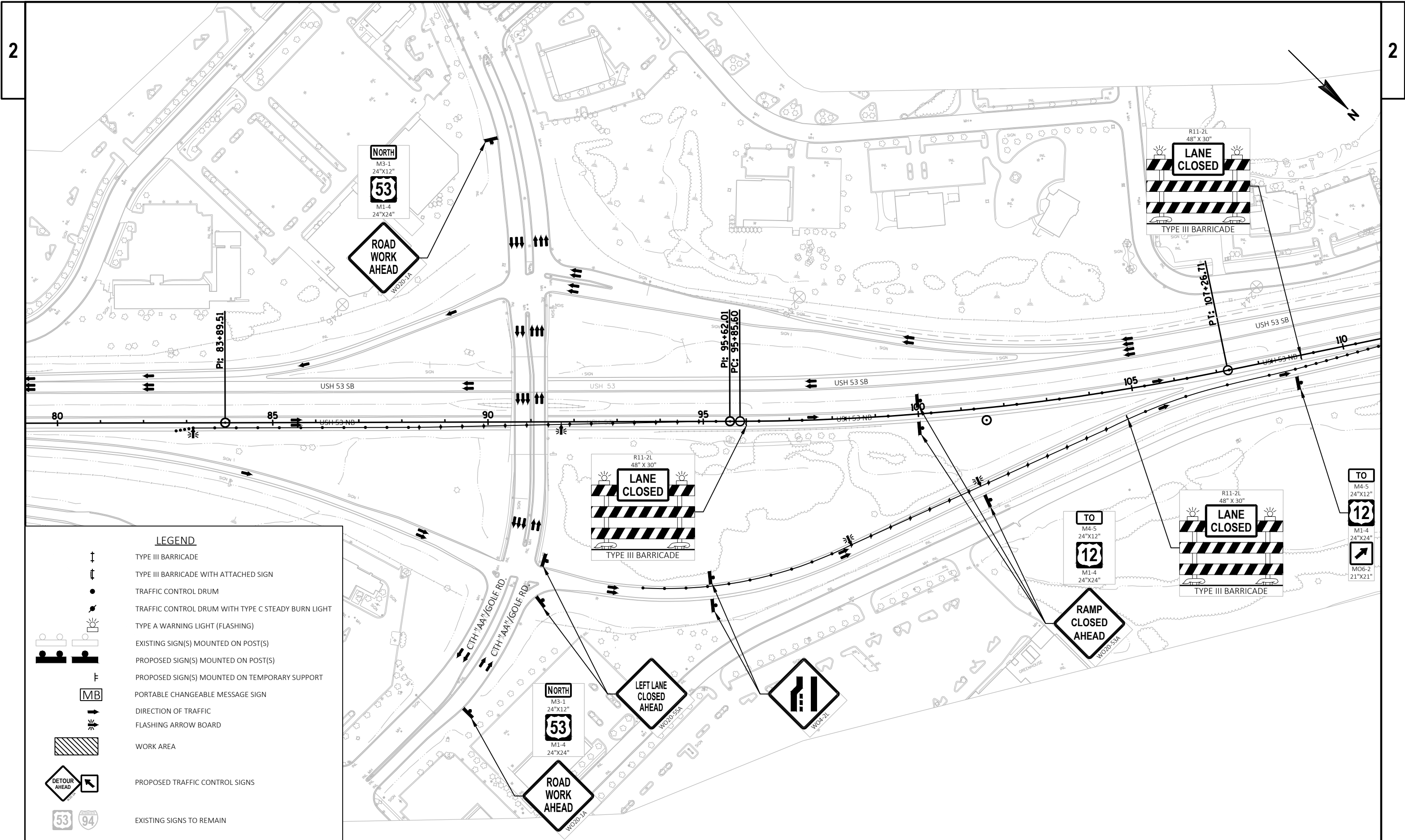
- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB LEFT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE RIGHT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY.





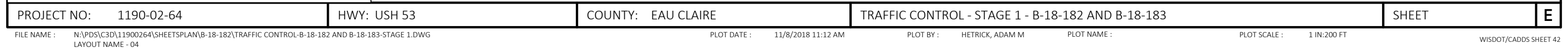






- USH 53 NB RIGHT LANE, SHOULDER, AND EXIT RAMP TO USH 12/CLAIREMONT AVENUE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB EXIT RAMP TO USH 12/CLAIREMONT AVENUE CLOSED DURING CONSTRUCTION. TRAFFIC WILL BE DETOURED USING THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY TO USH 12/CLAIREMONT AVENUE.
- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL OTHER RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB RIGHT LANE, SHOULDER, AND EXIT RAMP TO USH 12/CLAIREMONT AVENUE.
- BRIDGE DECK POLYMER OVERLAY OF THE LEFT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY AND THE CONCRETE MEDIAN ON THE BRIDGE.



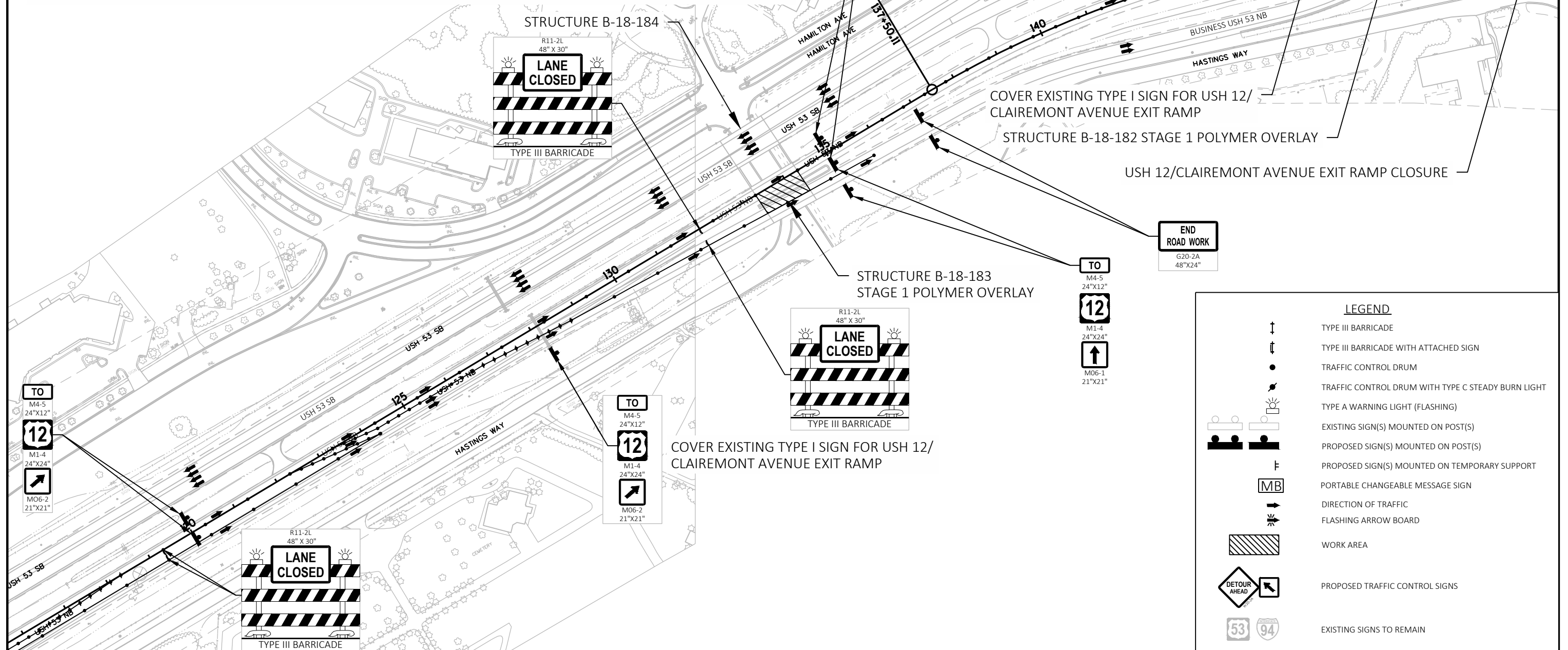


STRUCTURE B-18-182 AND B-18-183 STAGE 1 TRAFFIC CONTROL:

- USH 53 NB RIGHT LANE, SHOULDER, AND EXIT RAMP TO USH 12/CLAIREMONT AVENUE CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB EXIT RAMP TO USH 12/CLAIREMONT AVENUE CLOSED DURING CONSTRUCTION. TRAFFIC WILL BE DETOURED USING THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY TO USH 12/CLAIREMONT AVENUE.
- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL OTHER RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

STRUCTURE B-18-182 AND B-18-183 STAGE 1 CONSTRUCTION:

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB RIGHT LANE, SHOULDER, AND EXIT RAMP TO USH 12/CLAIREMONT AVENUE.
- BRIDGE DECK POLYMER OVERLAY OF THE LEFT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY AND THE CONCRETE MEDIAN ON THE BRIDGE.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 1 - B-18-182 AND B-18-183

SHEET

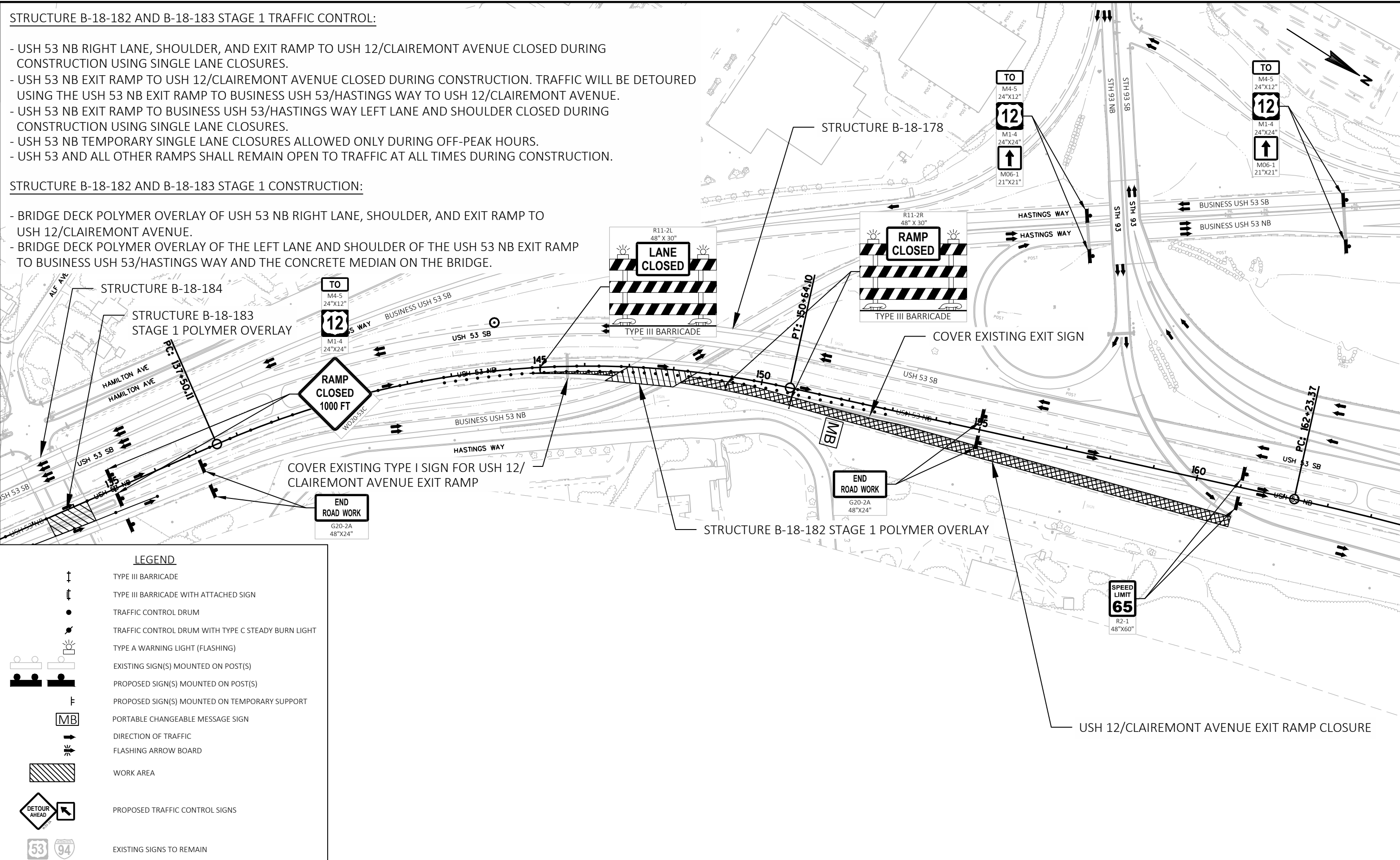
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STRUCTURE B-18-182 AND B-18-183 STAGE 1 TRAFFIC CONTROL:

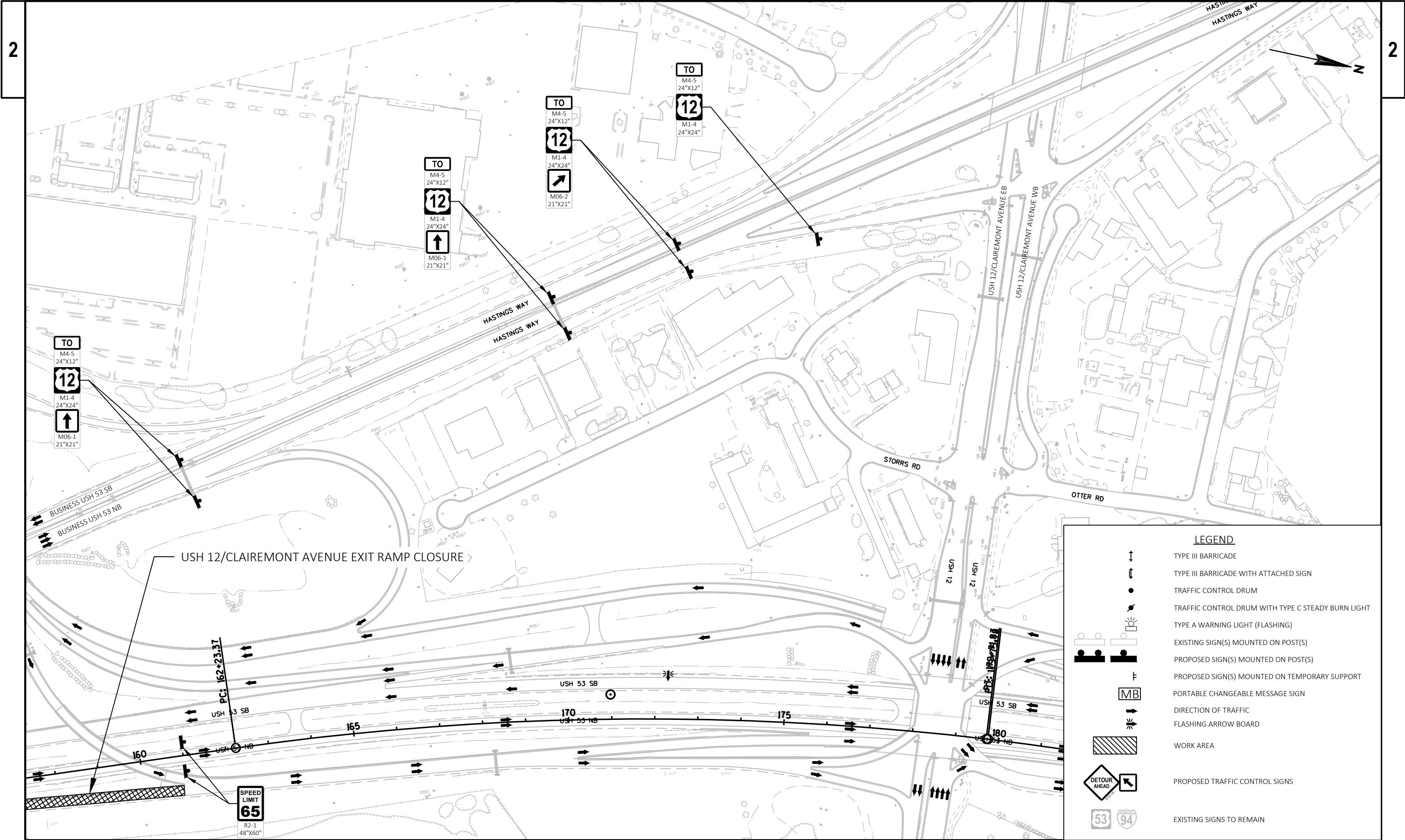
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- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL OTHER RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

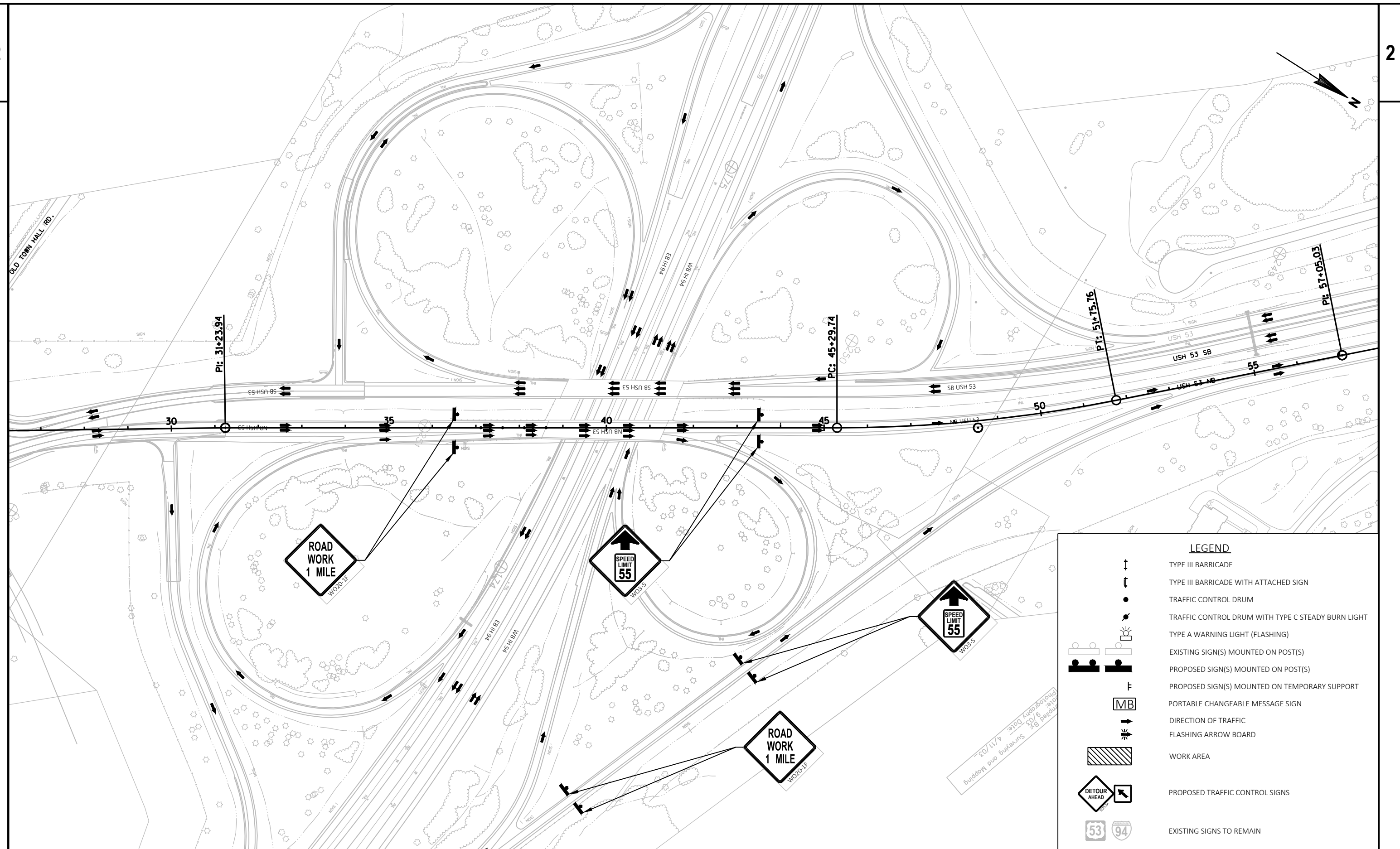
STRUCTURE B-18-182 AND B-18-183 STAGE 1 CONSTRUCTION:

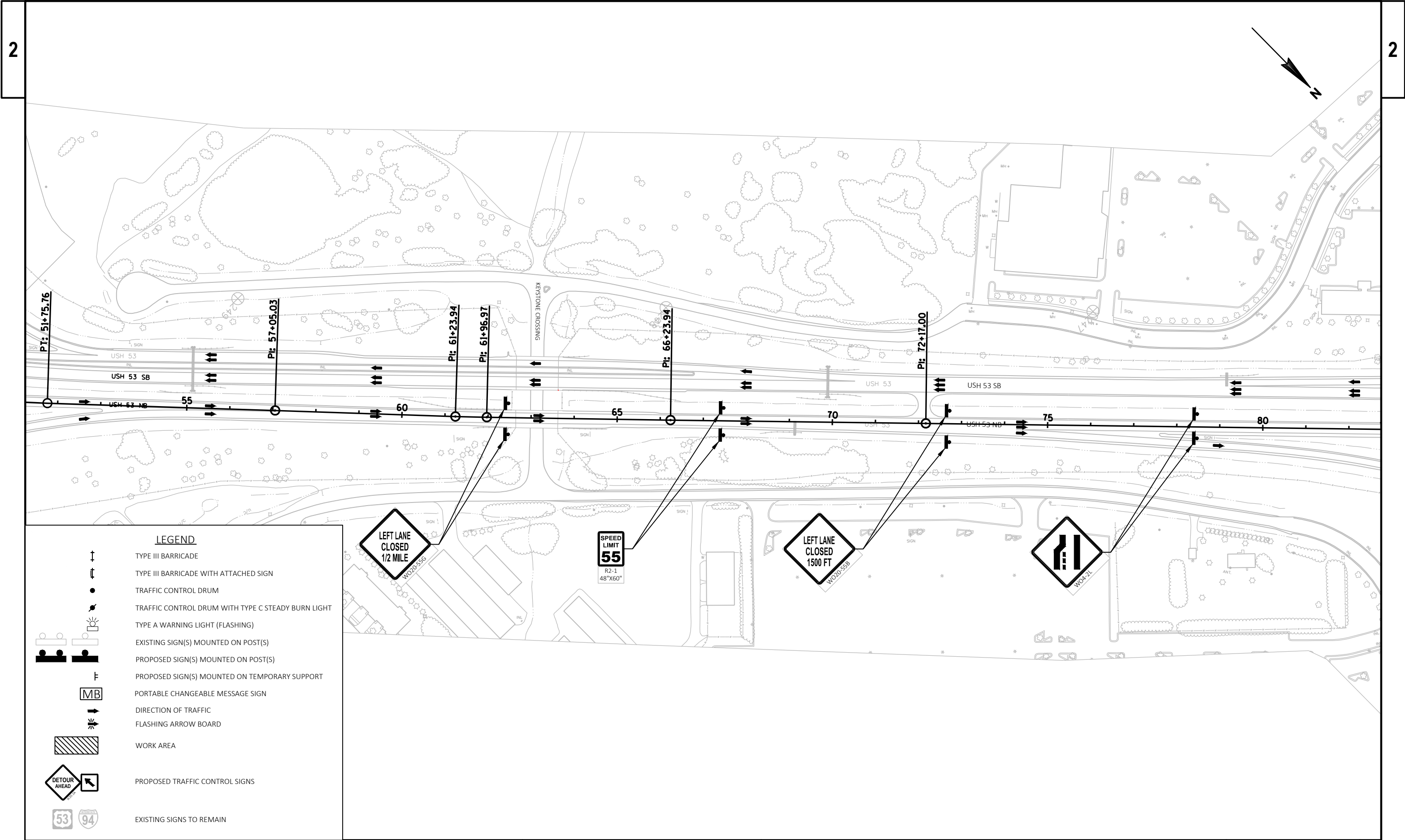
- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB RIGHT LANE, SHOULDER, AND EXIT RAMP TO USH 12/CLAIREMONT AVENUE.
- BRIDGE DECK POLYMER OVERLAY OF THE LEFT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY AND THE CONCRETE MEDIAN ON THE BRIDGE.

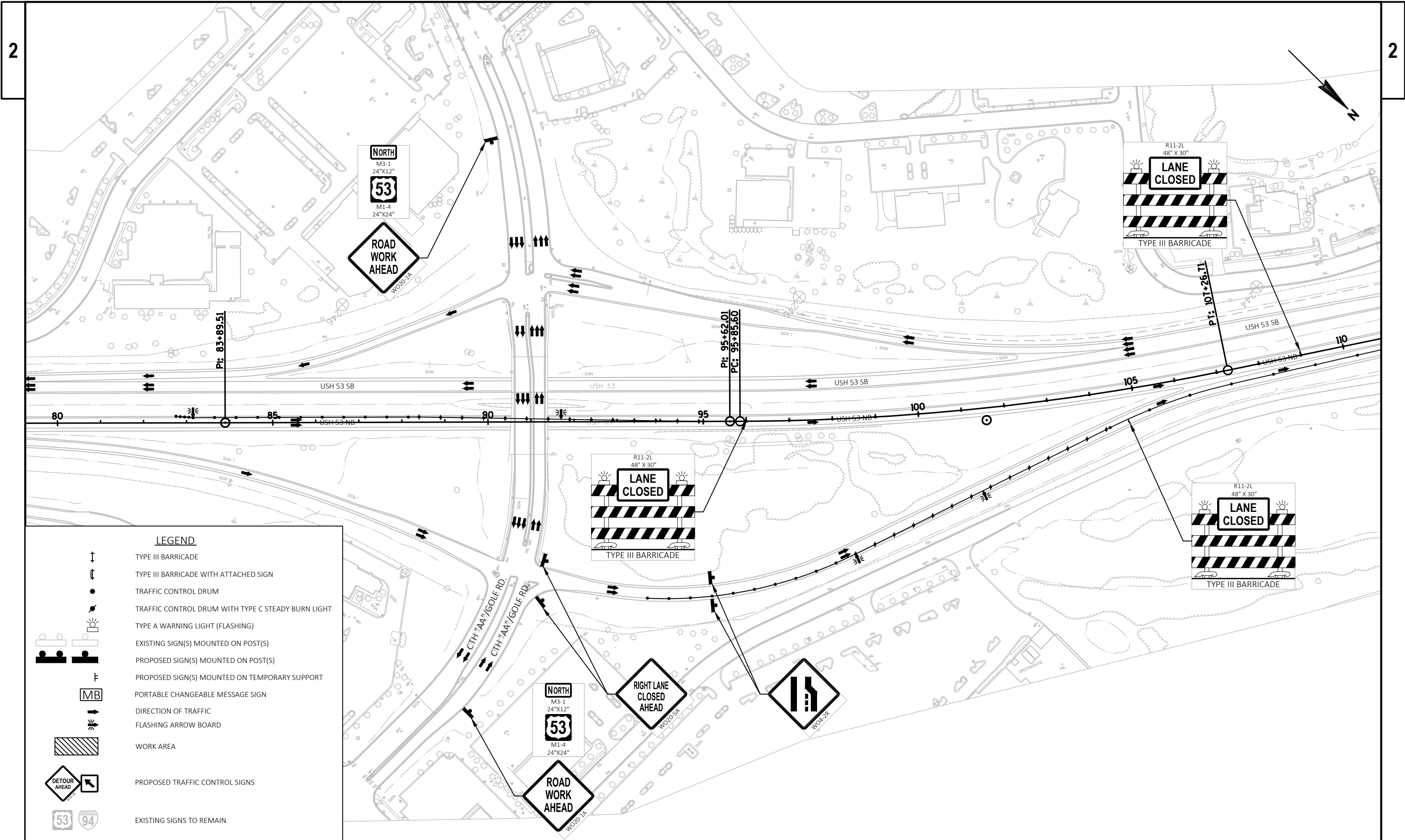










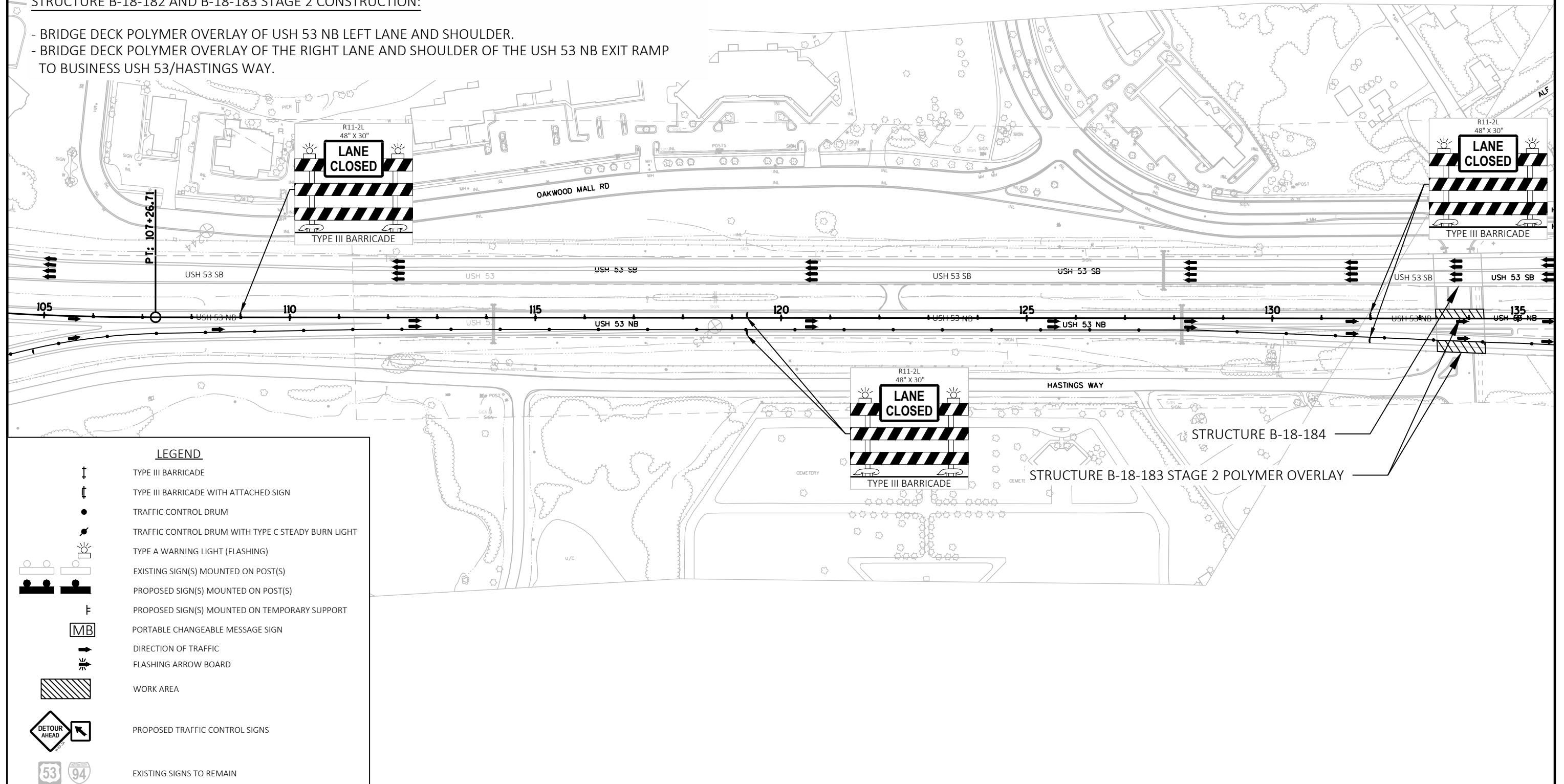


**STRUCTURE B-18-182 AND B-18-183 STAGE 2 TRAFFIC CONTROL:**

- USH 53 NB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

**STRUCTURE B-18-182 AND B-18-183 STAGE 2 CONSTRUCTION:**

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB LEFT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE RIGHT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY.



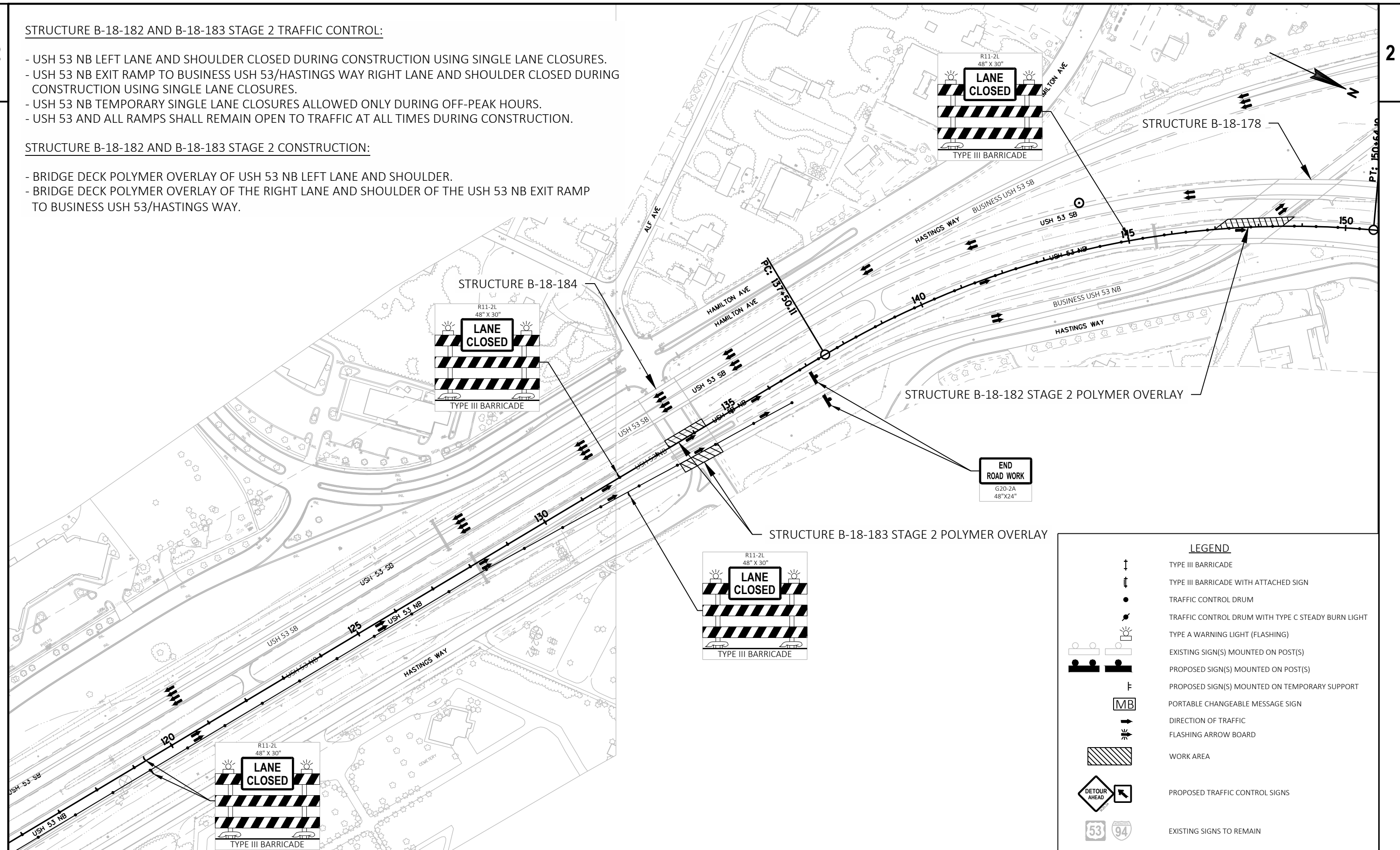


**STRUCTURE B-18-182 AND B-18-183 STAGE 2 TRAFFIC CONTROL:**

- USH 53 NB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
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- USH 53 AND ALL RAMPs SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

**STRUCTURE B-18-182 AND B-18-183 STAGE 2 CONSTRUCTION:**

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB LEFT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE RIGHT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY.



PROJECT NO: 1190-02-64

HWY: USH 53

COUNTY: EAU CLAIRE

TRAFFIC CONTROL - STAGE 2 - B-18-182 AND B-18-183

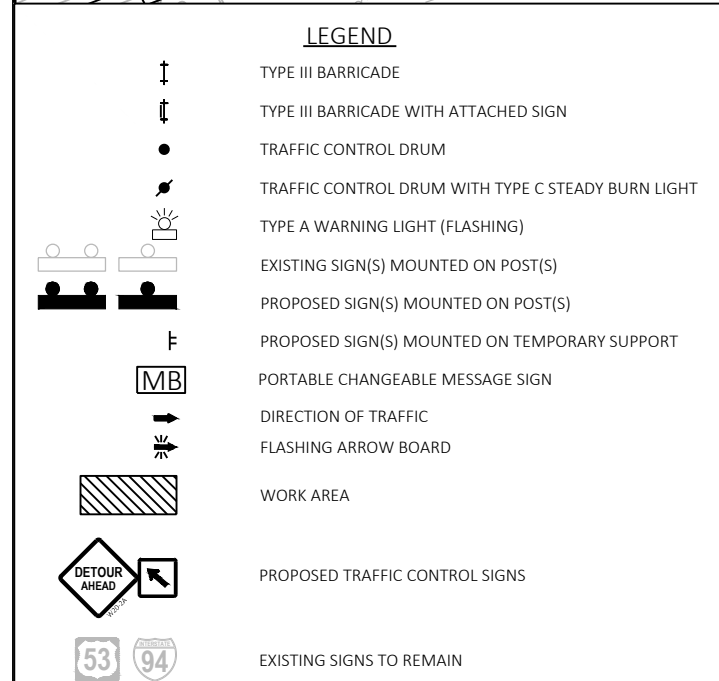
SHEET

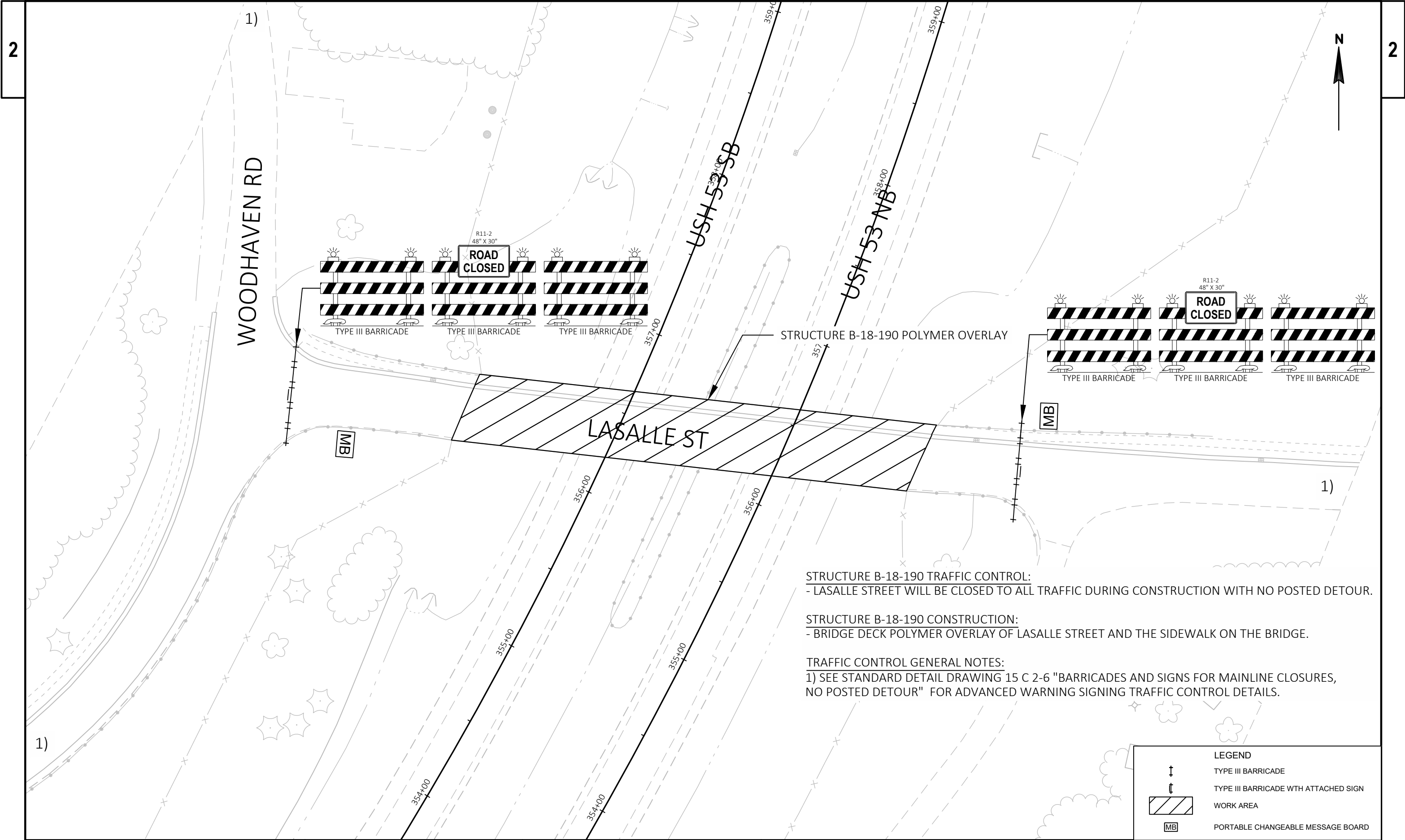
E



- USH 53 NB LEFT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY RIGHT LANE AND SHOULDER CLOSED DURING CONSTRUCTION USING SINGLE LANE CLOSURES.
- USH 53 NB TEMPORARY SINGLE LANE CLOSURES ALLOWED ONLY DURING OFF-PEAK HOURS.
- USH 53 AND ALL RAMPS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION.

- BRIDGE DECK POLYMER OVERLAY OF USH 53 NB LEFT LANE AND SHOULDER.
- BRIDGE DECK POLYMER OVERLAY OF THE RIGHT LANE AND SHOULDER OF THE USH 53 NB EXIT RAMP TO BUSINESS USH 53/HASTINGS WAY.





MB

PORTABLE CHANGEABLE MESSAGE BOARD

PHASE 1

LA SALLE\_  
\_STREET\_  
\_CLOSURE\_

PHASE 2

STARTING  
(DATE)  
\_\_\_\_\_

\*USE MESSAGE 7 DAYS PRIOR TO CONSTRUCTION

PHASE 1

LA SALLE\_  
\_STREET\_  
\_CLOSED\_

PHASE 2

\_\_\_USE\_\_\_  
\_\_\_ALT\_\_\_  
\_\_\_ROUTE\_

\*USE MESSAGE DURING CONSTRUCTION

Estimate Of Quantities

1190-02-64

Line	Item	Item Description	Unit	Total	Qty
0002	213.0100	Finishing Roadway (project) 01. 1190-02-64	EACH	1.000	1.000
0004	509.0301	Preparation Decks Type 1	SY	37.000	37.000
0006	509.0302	Preparation Decks Type 2	SY	17.000	17.000
0008	509.0310.S	Sawing Pavement Deck Preparation Areas	LF	370.000	370.000
0010	509.5100.S	Polymer Overlay	SY	17,246.000	17,246.000
0012	619.1000	Mobilization	EACH	1.000	1.000
0014	628.7010	Inlet Protection Type B	EACH	4.000	4.000
0016	642.5001	Field Office Type B	EACH	1.000	1.000
0018	643.0300	Traffic Control Drums	DAY	11,100.000	11,100.000
0020	643.0420	Traffic Control Barricades Type III	DAY	550.000	550.000
0022	643.0705	Traffic Control Warning Lights Type A	DAY	1,100.000	1,100.000
0024	643.0715	Traffic Control Warning Lights Type C	DAY	1,970.000	1,970.000
0026	643.0800	Traffic Control Arrow Boards	DAY	140.000	140.000
0028	643.0900	Traffic Control Signs	DAY	3,140.000	3,140.000
0030	643.0910	Traffic Control Covering Signs Type I	EACH	37.000	37.000
0032	643.0920	Traffic Control Covering Signs Type II	EACH	17.000	17.000
0034	643.1050	Traffic Control Signs PCMS	DAY	330.000	330.000
0036	643.5000	Traffic Control	EACH	1.000	1.000
0038	646.1020	Marking Line Epoxy 4-Inch	LF	8,005.000	8,005.000
0040	646.3020	Marking Line Epoxy 8-Inch	LF	930.000	930.000
0042	646.5020	Marking Arrow Epoxy	EACH	3.000	3.000
0044	646.5120	Marking Word Epoxy	EACH	2.000	2.000
0046	646.5420	Marking Aerial Enforcement Bar Epoxy	EACH	24.000	24.000
0048	646.9000	Marking Removal Line 4-Inch	LF	8,005.000	8,005.000
0050	646.9100	Marking Removal Line 8-Inch	LF	930.000	930.000
0052	646.9300	Marking Removal Special Marking	EACH	9.000	9.000
0054	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	1,200.000	1,200.000
0056	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0058	SPV.0035	Special 01. Rapid Set Deck Repair	CY	12.000	12.000

FINISHING ROADWAY (1190-02-64)			
CATEGORY	LOCATION	213.0100 EACH	REMARKS
0010	PROJECT 1190-02-64	1	USH 53
TOTAL 0010		1	

INLET PROTECTION TYPE B			
CATEGORY	LOCATION	628.7010 EACH	REMARKS
0010	B-18-168 EAU CLAIRE RIVER	4	USH 53 SB
TOTAL 0010		4	

FIELD OFFICE TYPE B			
CATEGORY	LOCATION	642.5001 EACH	REMARKS
0010	PROJECT 1190-02-64	1	USH 53
TOTAL 0010		1	

MOBILIZATION

CATEGORY	LOCATION	619.1000 EACH	REMARKS
0010	ROADWAY CONSTRUCTION	0.04	USH 53
TOTAL 0010		0.04	
0020	B-18-38	0.08	USH 53
TOTAL 0020		0.08	
0030	B-18-183	0.08	USH 53
TOTAL 0030		0.08	
0040	B-18-182	0.08	USH 53
TOTAL 0040		0.08	
0050	B-18-35	0.08	USH 53
TOTAL 0050		0.08	
0060	B-18-36	0.08	USH 53
TOTAL 0060		0.08	
0070	B-18-37	0.08	USH 53
TOTAL 0070		0.08	
0080	B-18-184	0.08	USH 53
TOTAL 0080		0.08	
0090	B-18-178	0.08	USH 53
TOTAL 0090		0.08	
0100	B-18-168	0.08	USH 53
TOTAL 0100		0.08	
0110	B-18-190	0.08	USH 53
TOTAL 0110		0.08	
0120	B-9-262	0.08	USH 53
TOTAL 0120		0.08	
0130	B-18-165	0.08	USH 53
TOTAL 0130		0.08	
PROJECT TOTAL		1.00	

TRAFFIC CONTROL				
CATEGORY	LOCATION	643.5000 EACH	REMARKS	
0010	PROJECT 1190-02-64	1	USH 53	
TOTAL 0010		1		

TRAFFIC CONTROL																							
			643.0300		643.0420		643.0705		643.0715		643.0800		643.0900		643.0910			643.0920			643.1050		
			TRAFFIC		TRAFFIC		TRAFFIC		TRAFFIC		TRAFFIC		TRAFFIC		TRAFFIC			TRAFFIC			TRAFFIC		
			CONTROL		CONTROL		CONTROL		CONTROL		CONTROL		CONTROL		CONTROL			CONTROL			CONTROL		
			DRUMS		BARRICADES		WARNING		WARNING		ARROW		SIGNS		COVERING			COVERING			SIGNS		
					TYPE III		LIGHTS		LIGHTS		BOARDS				SIGNS			SIGNS			PCMS		
							TYPE A		TYPE C						TYPE I			TYPE II					
CATEGORY	LOCATION	DAYS	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	CYCLES	SIGNS	EACH	CYCLES	SIGNS	EACH	DAYS	NO.	DAY
0010	B-9-262 STAGE 1	6	75	450	15	90	30	180	15	90	0	0	75	450	1	4	4	1	1	1	13	4	52
0010	B-18-35 STAGE 1	2	175	350	15	30	30	60	20	40	2	4	110	220	1	11	11	1	6	6	9	5	45
0010	B-18-35 STAGE 2	2	70	140	3	6	6	12	15	30	2	4	20	40	0	0	0	0	0	0	2	5	10
0010	B-18-36 STAGE 1	2	140	280	15	30	30	60	15	30	2	4	140	280	1	4	4	1	6	6	9	6	54
0010	B-18-36 STAGE 2	2	45	90	3	6	6	12	15	30	2	4	20	40	0	0	0	0	0	0	2	6	12
0010	B-18-37 STAGE 1	2	145	290	8	16	16	32	50	100	2	4	130	260	1	4	4	0	0	0	9	2	18
0010	B-18-37 STAGE 2	2	90	180	3	6	6	12	15	30	2	4	25	50	0	0	0	0	0	0	2	2	4
0010	B-18-38 STAGE 1	2	135	270	3	6	6	12	30	60	2	4	25	50	0	0	0	0	0	0	9	1	9
0010	B-18-38 STAGE 2	2	125	250	3	6	6	12	30	60	2	4	25	50	0	0	0	0	0	0	2	1	2
0010	B-18-165 STAGE 1	4	100	400	5	20	10	40	20	80	2	8	30	120	0	0	0	0	0	0	11	2	22
0010	B-18-165 STAGE 2	4	135	540	5	20	10	40	25	100	2	8	30	120	0	0	0	0	0	0	4	2	8
0010	B-18-168 STAGE 1	7	245	1715	8	56	16	112	40	280	2	14	30	210	0	0	0	0	0	0	14	1	14
0010	B-18-168 STAGE 2	7	225	1575	8	56	16	112	40	280	2	14	30	210	0	0	0	0	0	0	7	1	7
0010	B-18-178 AND B-18-184 STAGE 1	2	700	1400	18	36	36	72	120	240	8	16	90	180	0	0	0	0	0	0	9	2	18
0010	B-18-178 AND B-18-184 STAGE 2	2	660	1320	18	36	36	72	100	200	8	16	90	180	0	0	0	0	0	0	2	2	4
0010	B-18-182 AND B-18-183 STAGE 1	2	560	1120	25	50	50	100	100	200	8	16	220	440	2	7	14	2	2	4	9	3	27
0010	B-18-182 AND B-18-183 STAGE 2	2	340	680	20	40	40	80	60	120	8	16	80	160	0	0	0	0	0	0	2	3	6
0010	B-18-190 STAGE 1	2	25	50	20	40	40	80	0	0	0	0	40	80	0	0	0	0	0	0	9	2	18
TOTAL 0010			11100		550		1100		1970		140		3140		37			17			330		



MARKING LINE EPOXY 4-INCH

CATEGORY	LOCATION	646.1020 LF	WHITE LF	YELLOW LF	REMARKS
0010	B-09-262	1000	500	500	MELBY STREET
0010	B-18-35	710	435	275	USH 53 SB
0010	B-18-36	735	455	280	USH 53 NB
0010	B-18-37	425	225	200	USH 53 SB
0010	B-18-38	238	138	100	USH 53 NB
0010	B-18-165	605	125	480	RIVER PRAIRIE DRIVE
0010	B-18-168	2250	1250	1000	USH 53 SB
0010	B-18-178	338	188	150	USH 53 SB
0010	B-18-182	395	240	155	USH 53 NB
0010	B-18-183	250	150	100	USH 53 NB
0010	B-18-184	250	150	100	USH 53 SB
0010	B-18-190	810	270	540	LA SALLE STREET
TOTAL 0010		8005			

MARKING ARROW EPOXY

CATEGORY	LOCATION	646.5020 EACH	REMARKS
0010	B-09-262	1	MELBY STREET
0010	B-18-165	2	RIVER PRAIRIE DRIVE
TOTAL 0010		3	

MARKING LINE EPOXY 8-INCH

CATEGORY	LOCATION	646.3020 LF	REMARKS
0010	B-09-262	100	MELBY STREET
0010	B-18-36	10	USH 53 NB
0010	B-18-165	370	RIVER PRAIRIE DRIVE
0010	B-18-182	50	USH 53 NB
0010	B-18-183	200	USH 53 NB
0010	B-18-184	200	USH 53 SB
TOTAL 0010		930	

MARKING WORD EPOXY

CATEGORY	LOCATION	646.5120 EACH	REMARKS
0010	B-18-165 RIVER PRAIRIE DRIVE	2	ONLY
TOTAL 0010		2	

MARKING AERIAL ENFORCEMENT BAR EPOXY

CATEGORY	LOCATION	646.5420 LF	REMARKS
0010	B-18-168 EAU CLAIRE RIVER USH 53 SB	24	STATE PATROL TO FIELD LOCATE
TOTAL 0010		24	

MARKING REMOVAL LINE 8-INCH

CATEGORY	LOCATION	646.9100 LF	REMARKS
0010	B-09-262	100	MELBY STREET
0010	B-18-36	10	USH 53 NB
0010	B-18-165	370	RIVER PRAIRIE DRIVE
0010	B-18-182	50	USH 53 NB
0010	B-18-183	200	USH 53 NB
0010	B-18-184	200	USH 53 SB
TOTAL 0010		930	

MARKING REMOVAL LINE 4-INCH

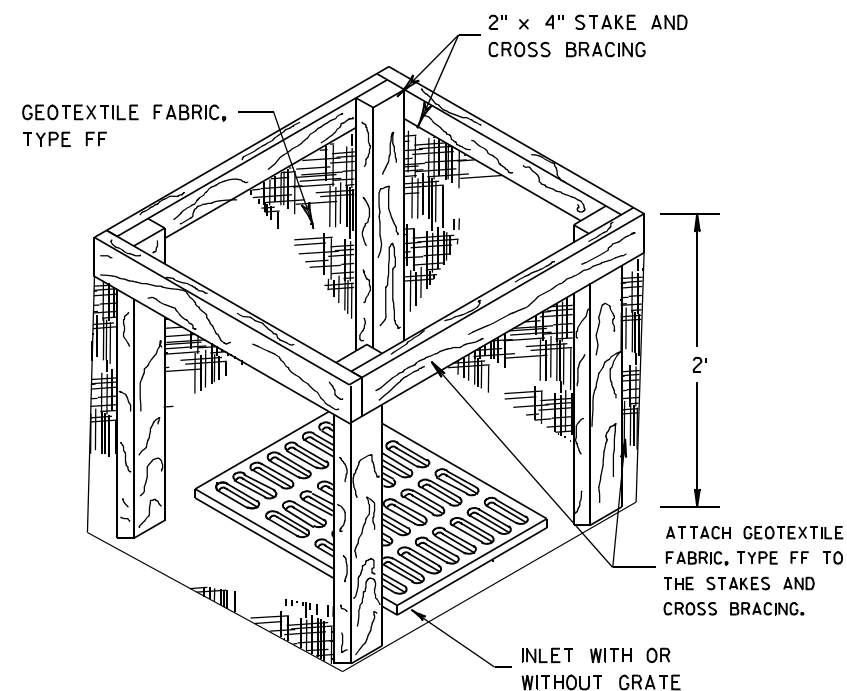
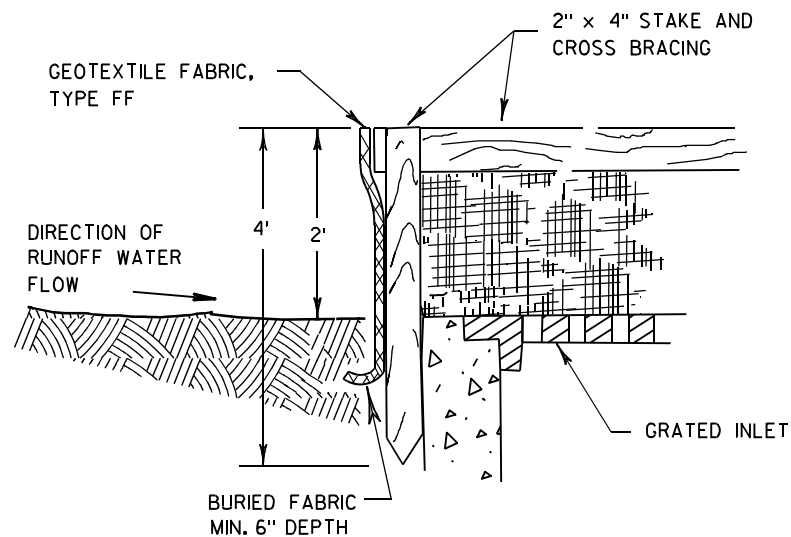
CATEGORY	LOCATION	646.9000 LF	WHITE LF	YELLOW LF	REMARKS
0010	B-09-262	1000	500	500	MELBY STREET
0010	B-18-35	710	435	275	USH 53 SB
0010	B-18-36	735	455	280	USH 53 NB
0010	B-18-37	425	225	200	USH 53 SB
0010	B-18-38	238	138	100	USH 53 NB
0010	B-18-165	605	125	480	RIVER PRAIRIE DRIVE
0010	B-18-168	2250	1250	1000	USH 53 SB
0010	B-18-178	338	188	150	USH 53 SB
0010	B-18-182	395	240	155	USH 53 NB
0010	B-18-183	250	150	100	USH 53 NB
0010	B-18-184	250	150	100	USH 53 SB
0010	B-18-190	810	270	540	LA SALLE STREET
TOTAL 0010		8005			

MARKING REMOVAL SPECIAL MARKING

CATEGORY	LOCATION	646.9300 EACH	REMARKS
0010	B-09-262 MELBY STREET	1	ARROW
0010	B-18-165 RIVER PRAIRIE DRIVE	2	ARROW
0010	B-18-165 RIVER PRAIRIE DRIVE	2	ONLY
0010	B-18-168 EAU CLAIRE RIVER	4	AERIAL ENFORCEMENT BAR
TOTAL 0010		9	

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C06-09	SIGNING & MARKING FOR TWO LANE BRIDGES
15C07-14B	PAVEMENT MARKING WORDS
15C07-14C	PAVEMENT MARKING ARROWS
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15C08-19B	PAVEMENT MARKING (TURN LANES)
15C08-19C	PAVEMENT MARKING (TURN LANES)
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C14-03	AERIAL ENFORCEMENT BARS PAVEMENT MARKING DETAILS
15C19-05C	MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY
15C31-03A	PAVEMENT MARKING (RAMPS AND GORES)
15C31-03B	PAVEMENT MARKING MAJOR SPLIT FREEWAY TO FREEWAY
15C31-03C	PAVEMENT MARKING FOR PARALLEL ON-RAMP AND PARALLEL OFF-RAMP
15D12-06A	TRAFFIC CONTROL, LANE CLOSURE
15D12-06B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION
15D15-04A	TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-04B	TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-04C	TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-04D	TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-04E	TRAFFIC CONTROL, PARALLEL EXIT RAMP WITHIN LANE CLOSURE
15D16-03	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D20-04	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D21-06	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



**INLET PROTECTION, TYPE A**

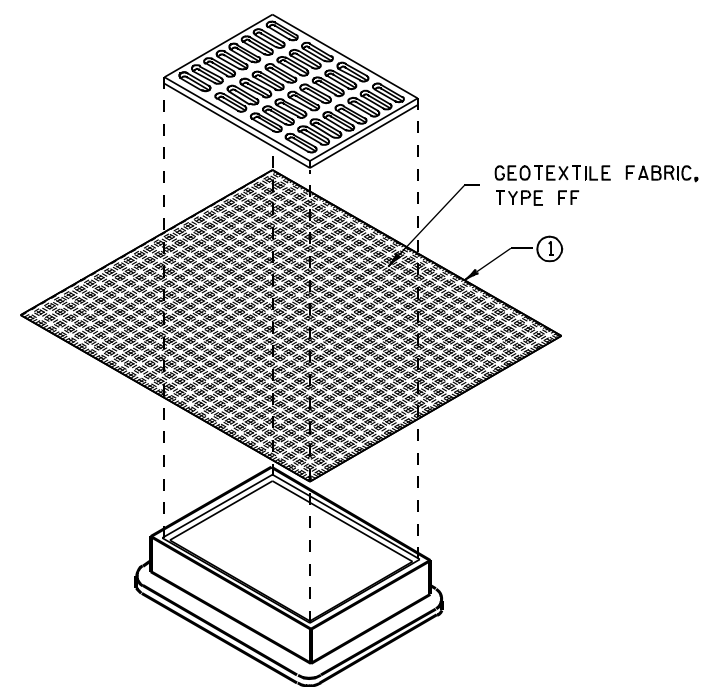
**GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

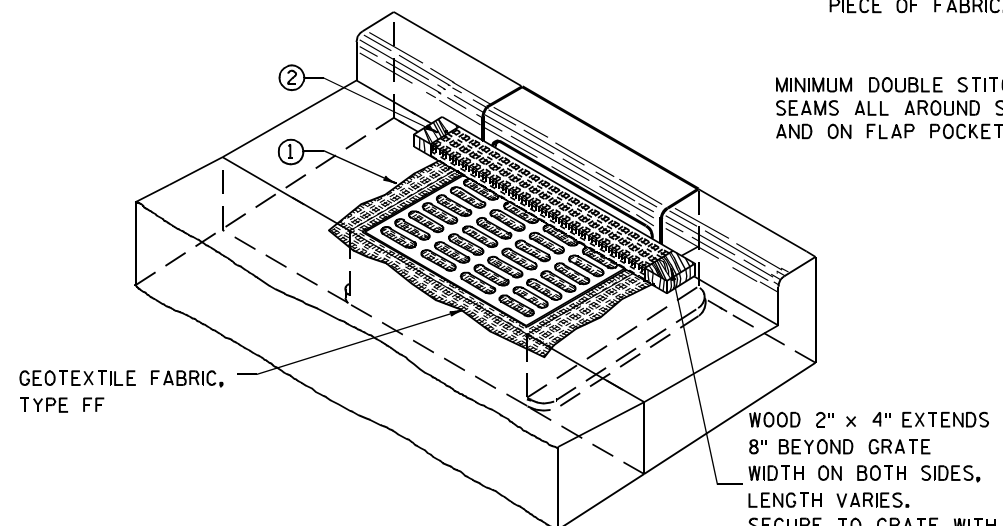
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B  
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



**INLET PROTECTION, TYPE C (WITH CURB BOX)**

**INSTALLATION NOTES**

**TYPE B & C**

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

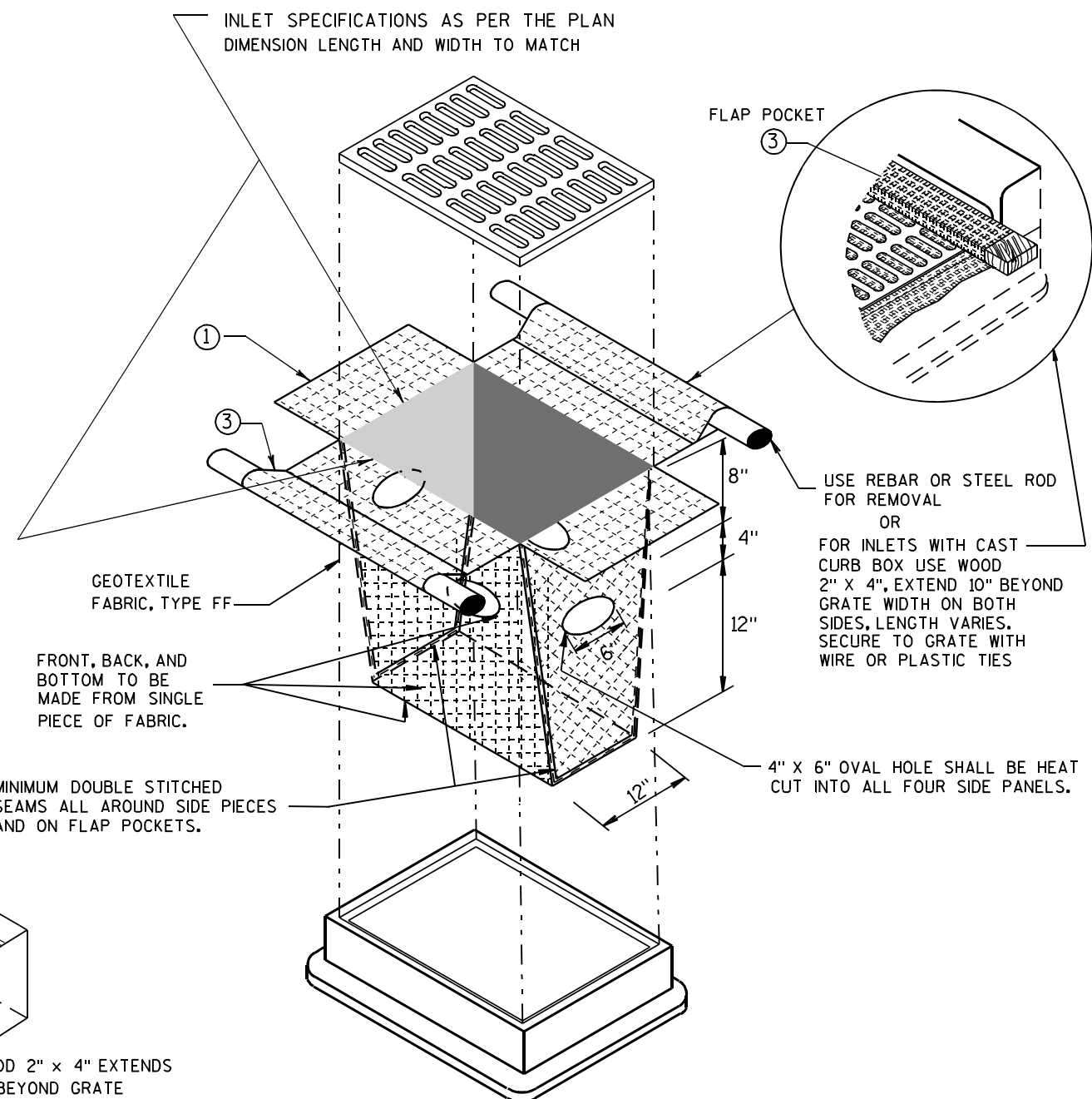
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

**TYPE D**

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



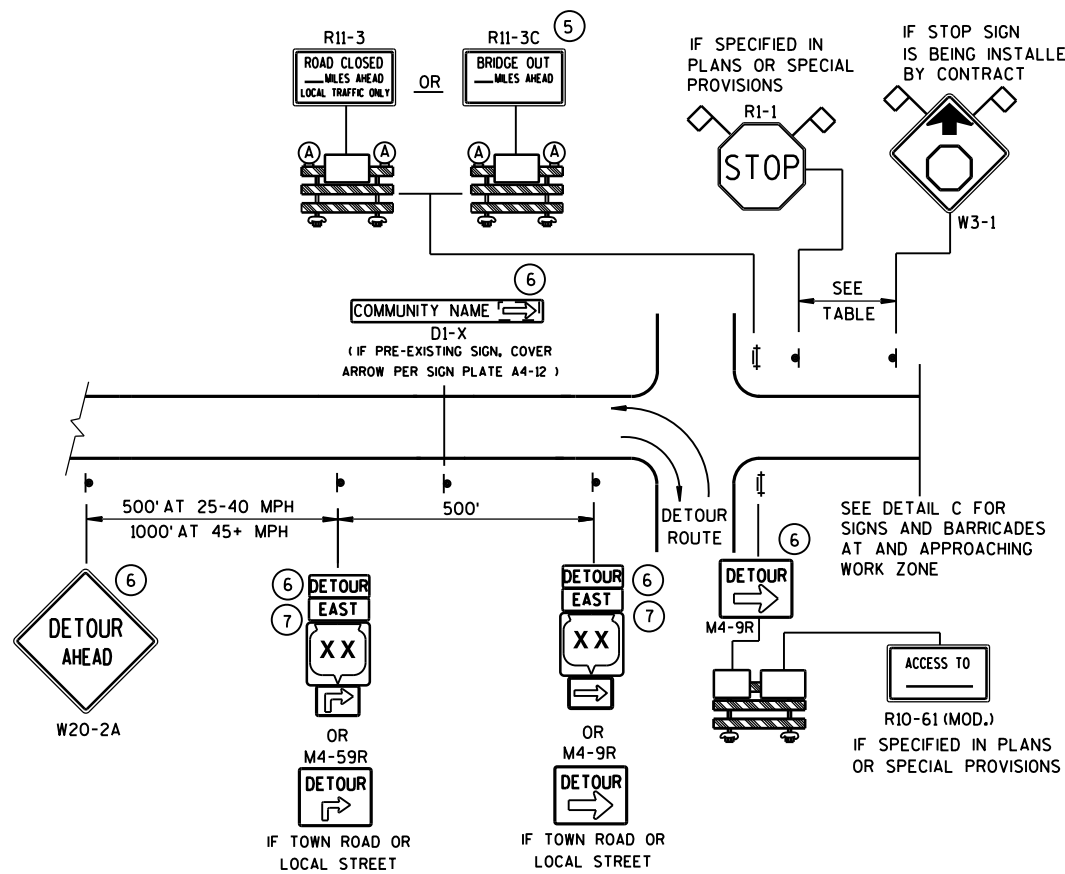
**INLET PROTECTION, TYPE D**

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

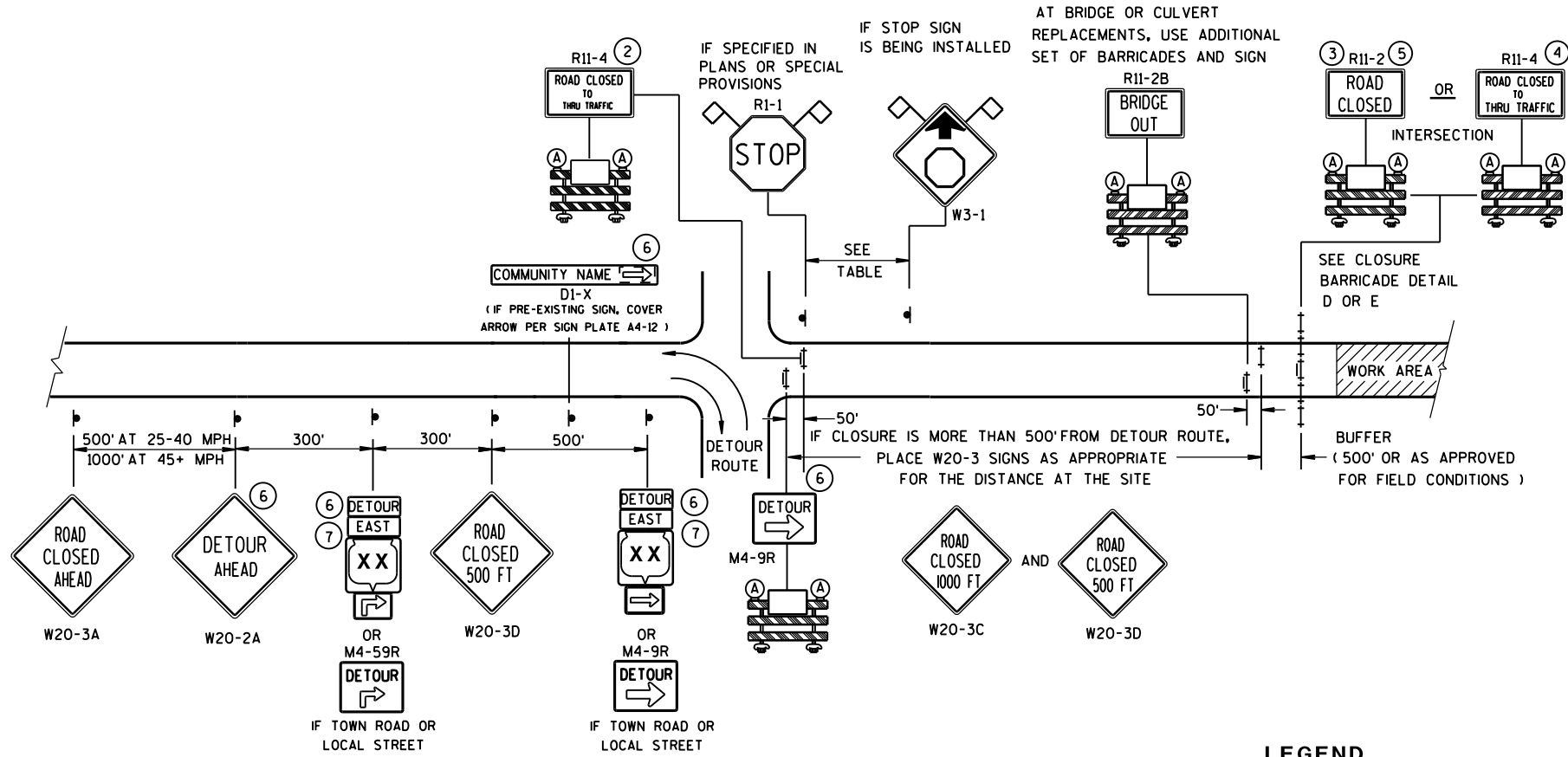
**INLET PROTECTION  
TYPE A, B, C, AND D**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

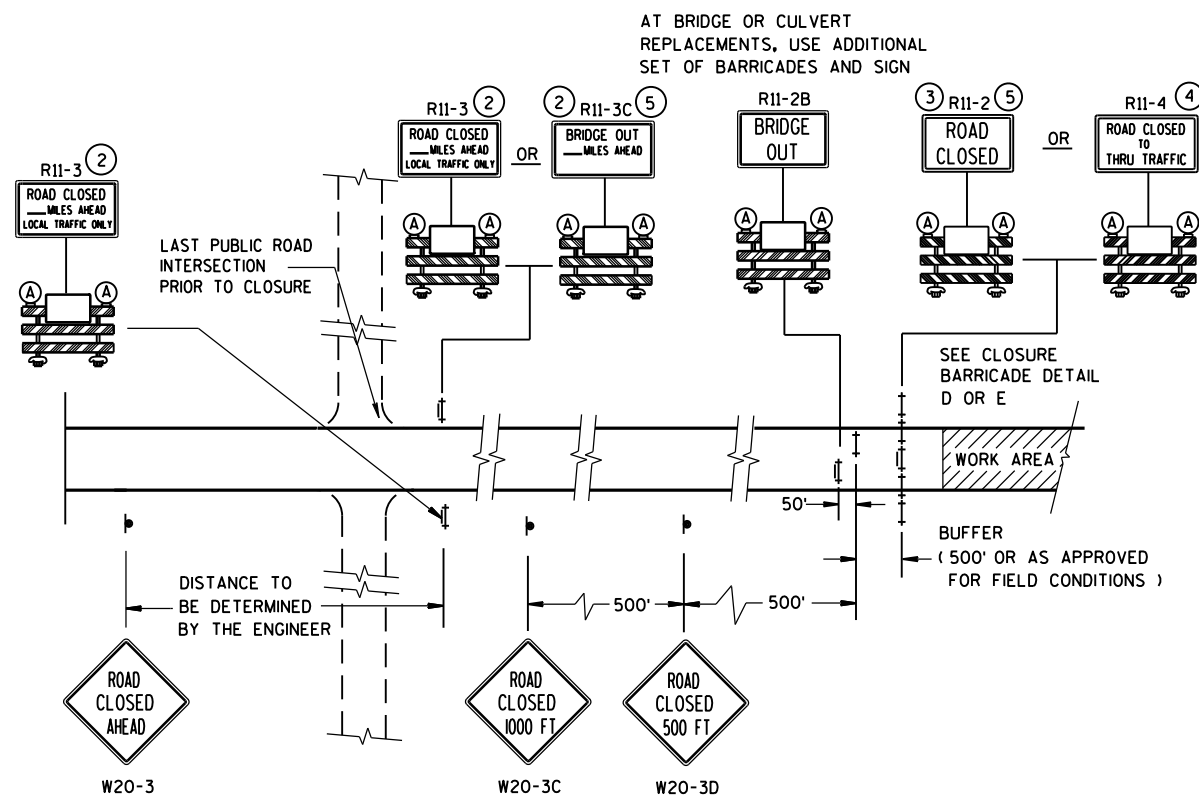
APPROVED  
10/16/02 /S/ Beth Cannestra  
DATE  
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER



DETAIL A  
MAINLINE CLOSURE WITH POSTED DETOUR  
WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B  
MAINLINE CLOSURE WITH POSTED DETOUR  
WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C  
MAINLINE CLOSURE, NO POSTED DETOUR

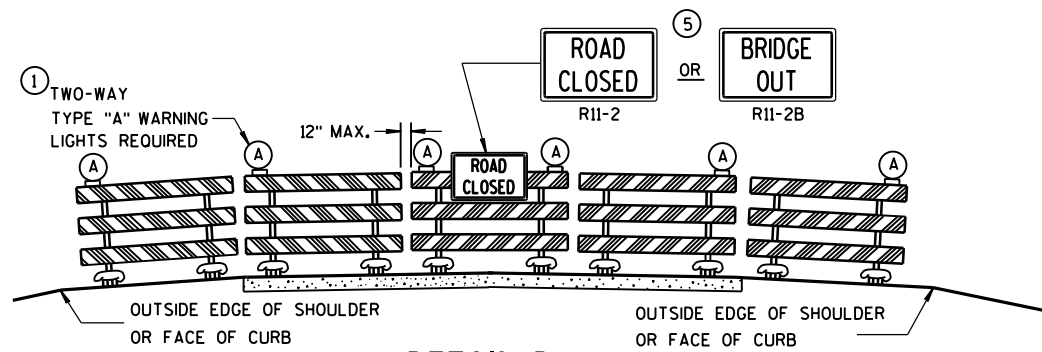
LEGEND

- SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- Ⓐ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA
- DETOUR EAST M4-8 M3-X
- XX OR COUNTY XX OR XX M1-4 M1-5A M1-6
- OR M05-1 M06-1
- ◇ ◇ FLAGS, 16" X 16" MIN., (ORANGE)

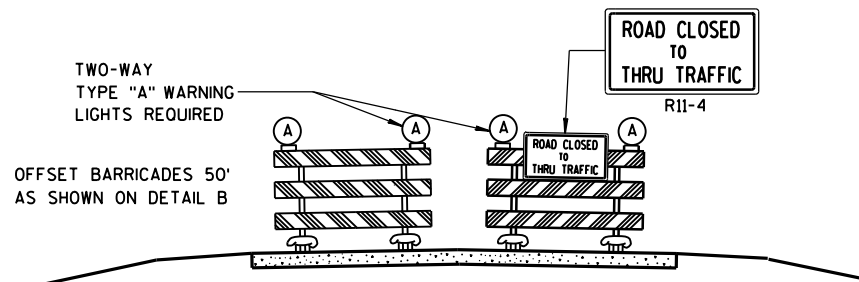
SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Sept. 2015 DATE	/S/ Peter Amokobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER



DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW



DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

### GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

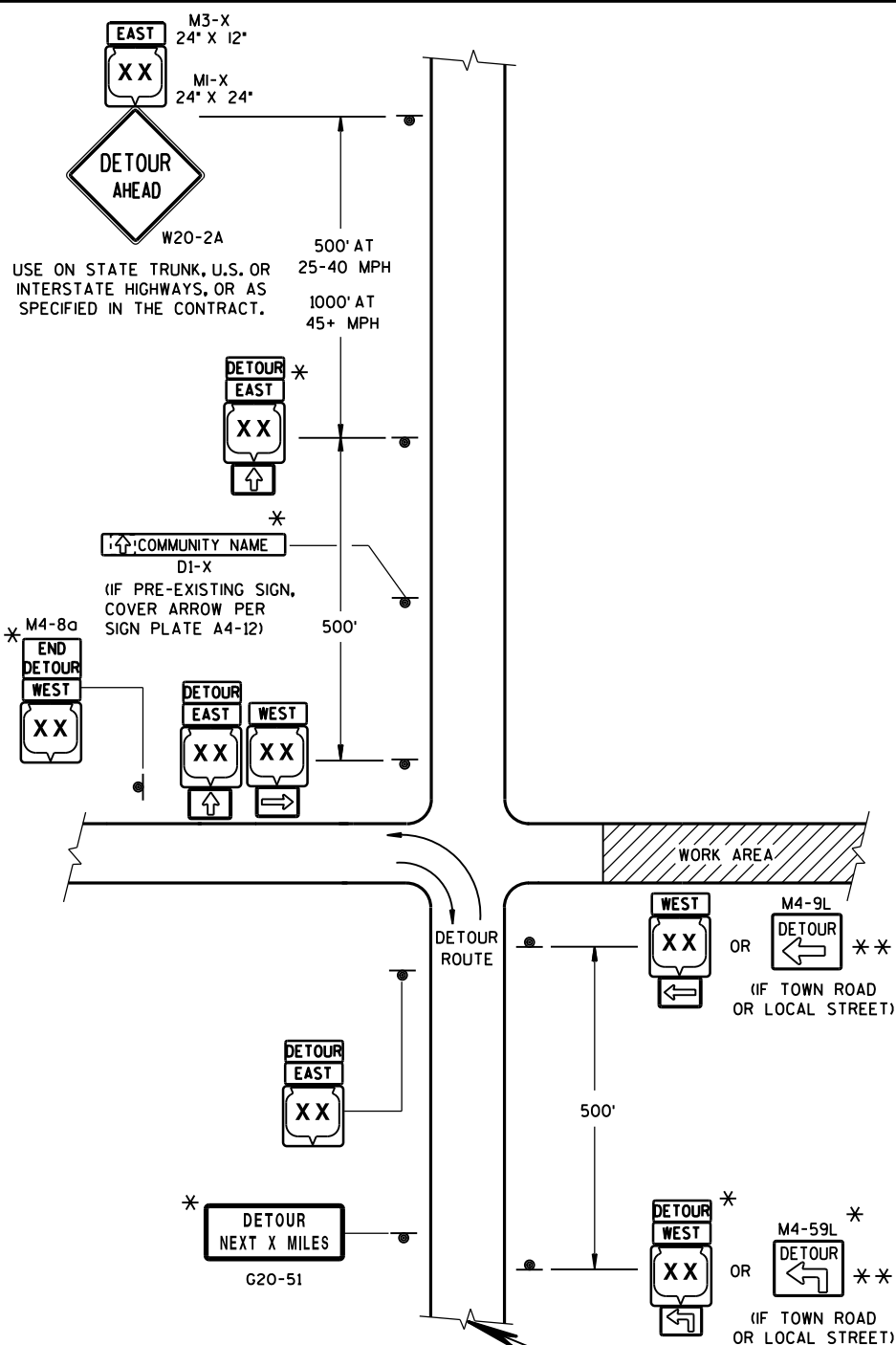
ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

- 1 TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- 2 THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- 3 FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- 4 FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- 5 FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- 6 INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- 7 "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Sept. 2015 DATE	/S/ Peter Amokobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	





**LEGEND**

SIGN ON PERMANENT SUPPORT

WORK AREA

M4-8  
 M3-X

OR OR   
M1-4 M1-5A M1-6

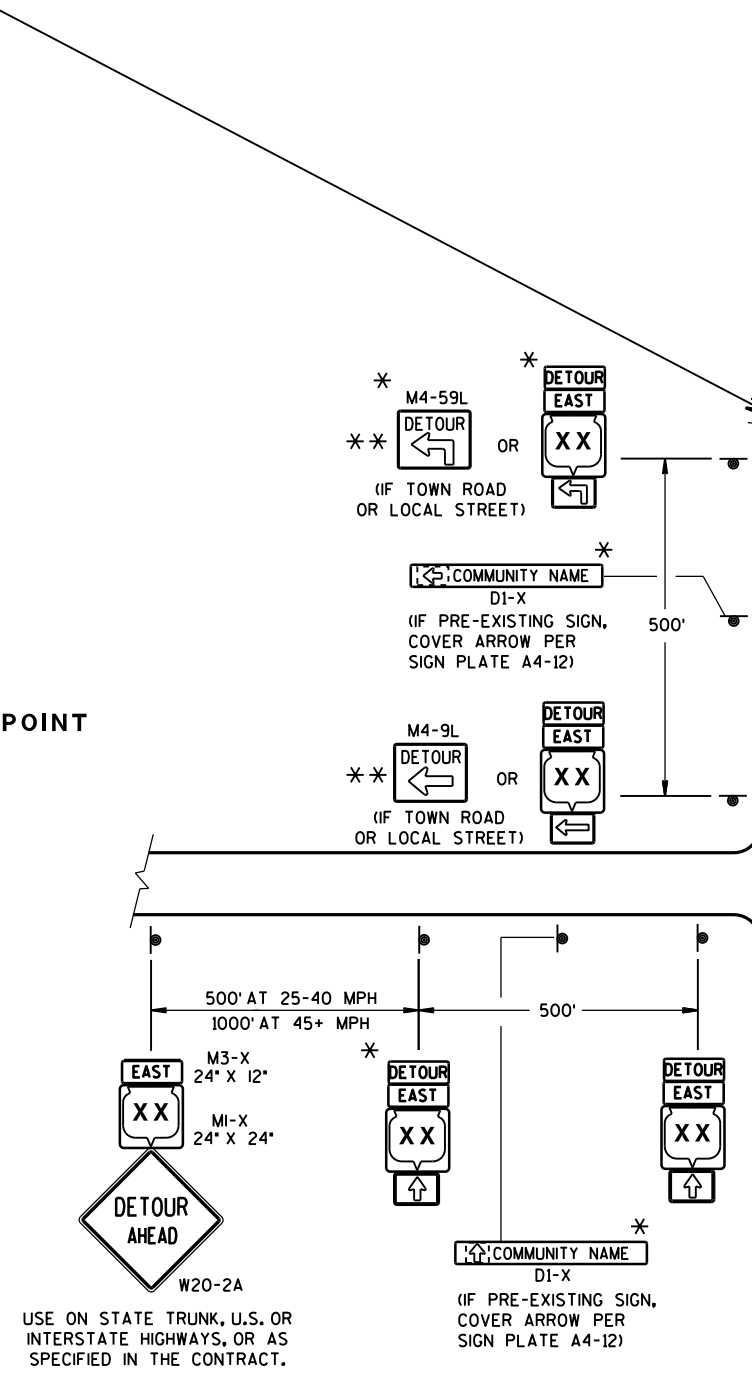
OR OR   
M05-1 M06-1 M06-1

SEE SPECIFIC PROJECT DETOUR  
SIGNING DETAIL SHEETS AND  
DETAIL A OR B ON SDD 15C2-SHEET "a"

THIS DRAWING PROVIDES GENERAL GUIDANCE  
ON TYPICAL DETOUR SIGN LAYOUT AND SPACING.  
SEE PROJECT DETOUR SIGNING SHEETS FOR  
SPECIFIC DETAILS FOR EACH PROJECT.

MATCH POINT

DETAIL F  
DETOUR SIGNING



**GENERAL NOTES**

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS, MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

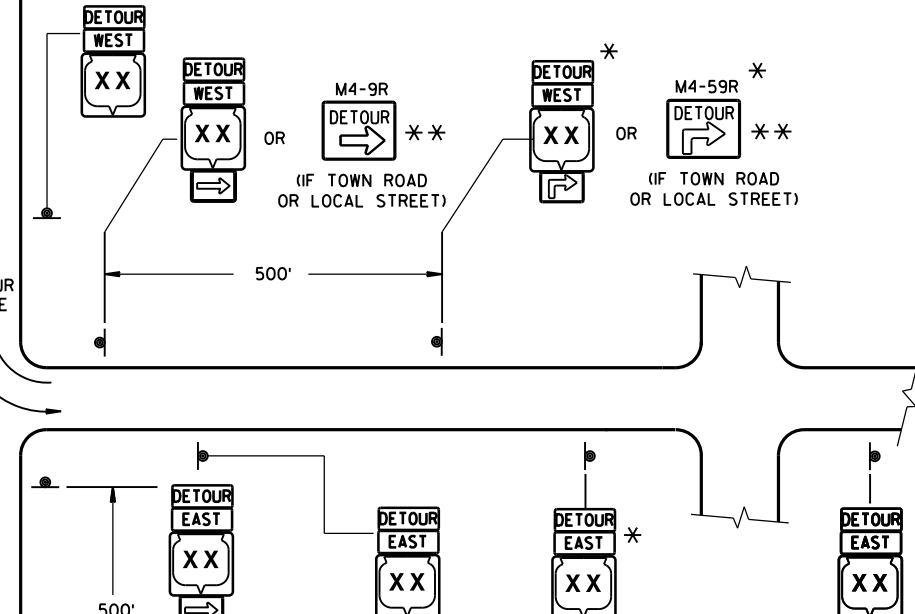
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-9 SHALL BE 30" X 24".
- M4-8a SHALL BE 24" X 18".
- G20-51 SHALL BE 60" X 24".
- W20-2 SHALL BE 48" X 48".
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

\* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

\*\* FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

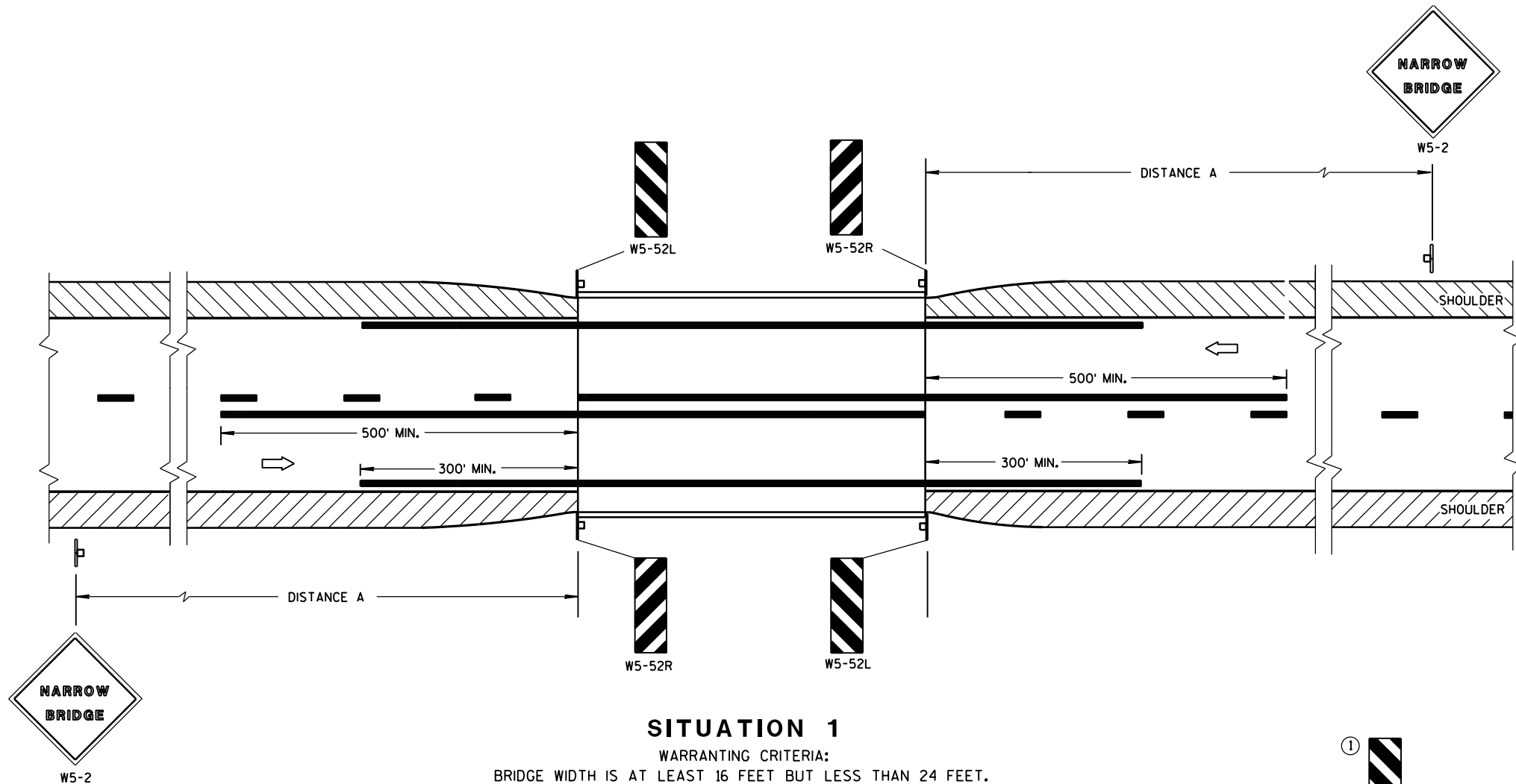


PLACE SIGNS BEYOND INTERSECTIONS WITH  
STATE OR COUNTY TRUNK HIGHWAYS OR  
AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF  
URBAN AREA.)

**DETOUR SIGNING FOR  
MAINLINE CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE Sept. 2015 /S/ Peter Amakobe Atepe  
STATEWIDE WORK ZONE TRAFFIC  
SAFETY ENGINEER  
FHWA



### SITUATION 1

WARRANTING CRITERIA:  
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

### GENERAL NOTES

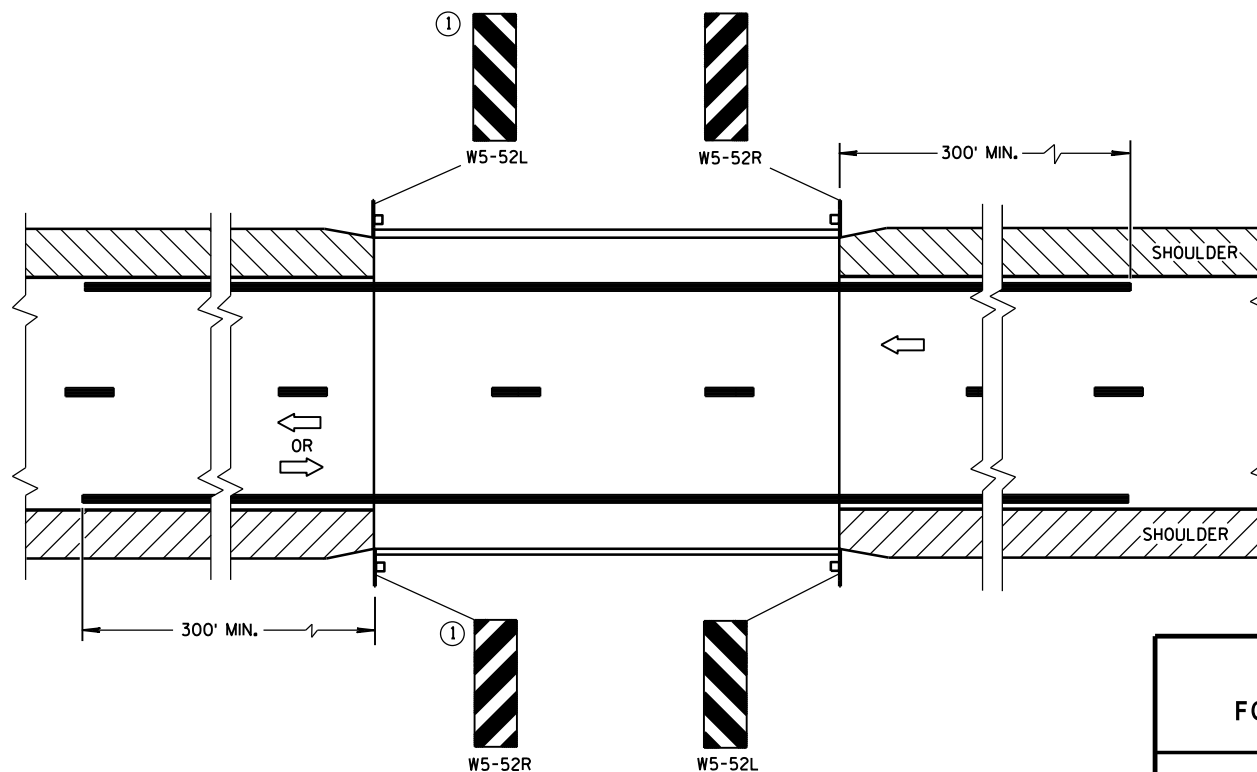
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

① OMIT ON ONE-WAY TRAVELLED WAYS.

➡ DIRECTION OF TRAFFIC



### SITUATION 2

WARRANTING CRITERIA:  
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND  
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

### SIGNING & MARKING FOR TWO LANE BRIDGES

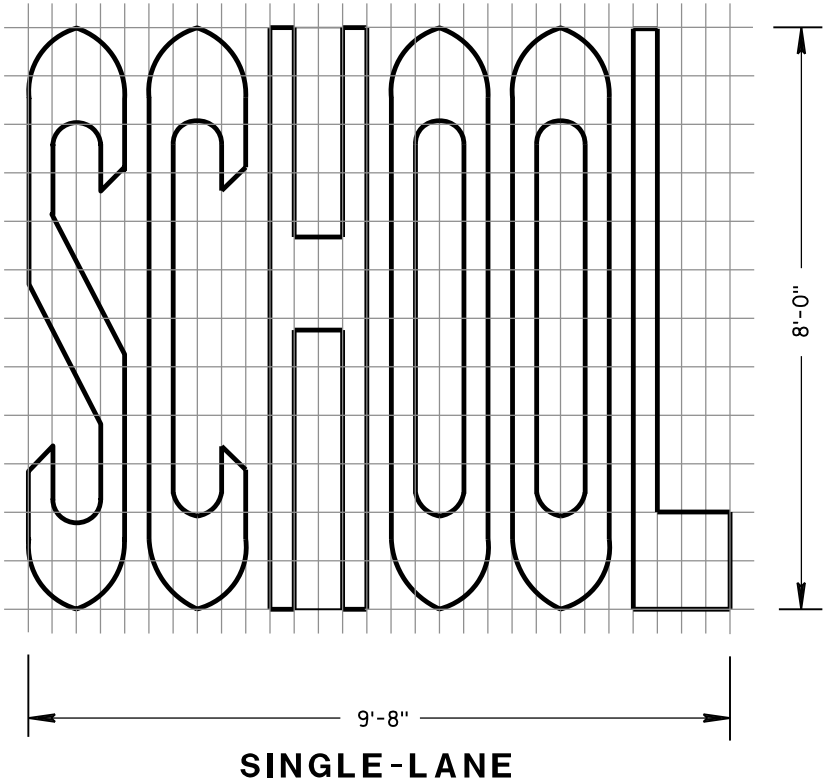
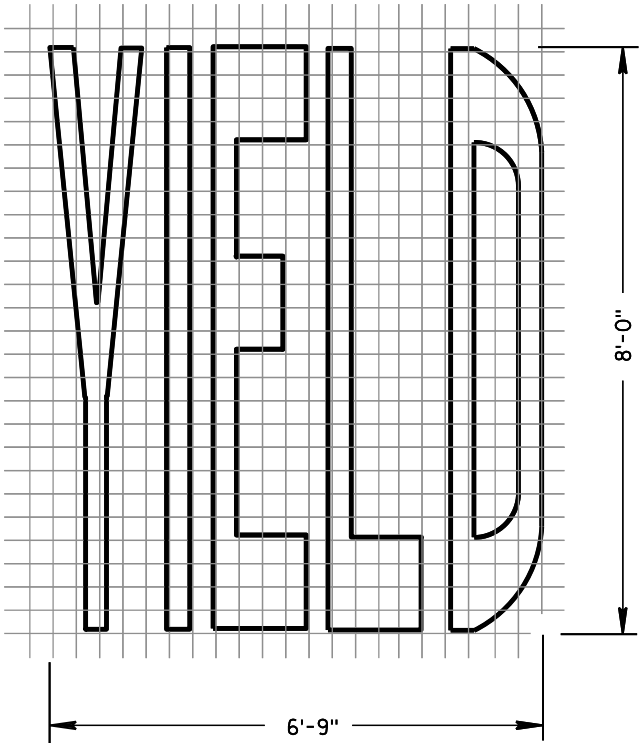
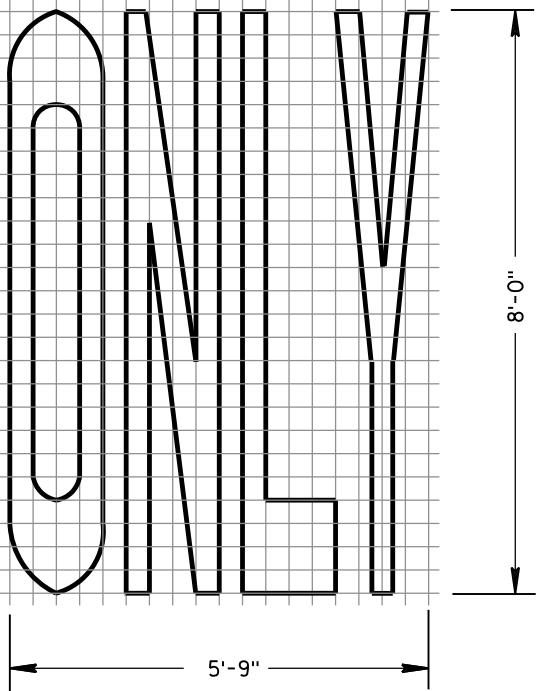
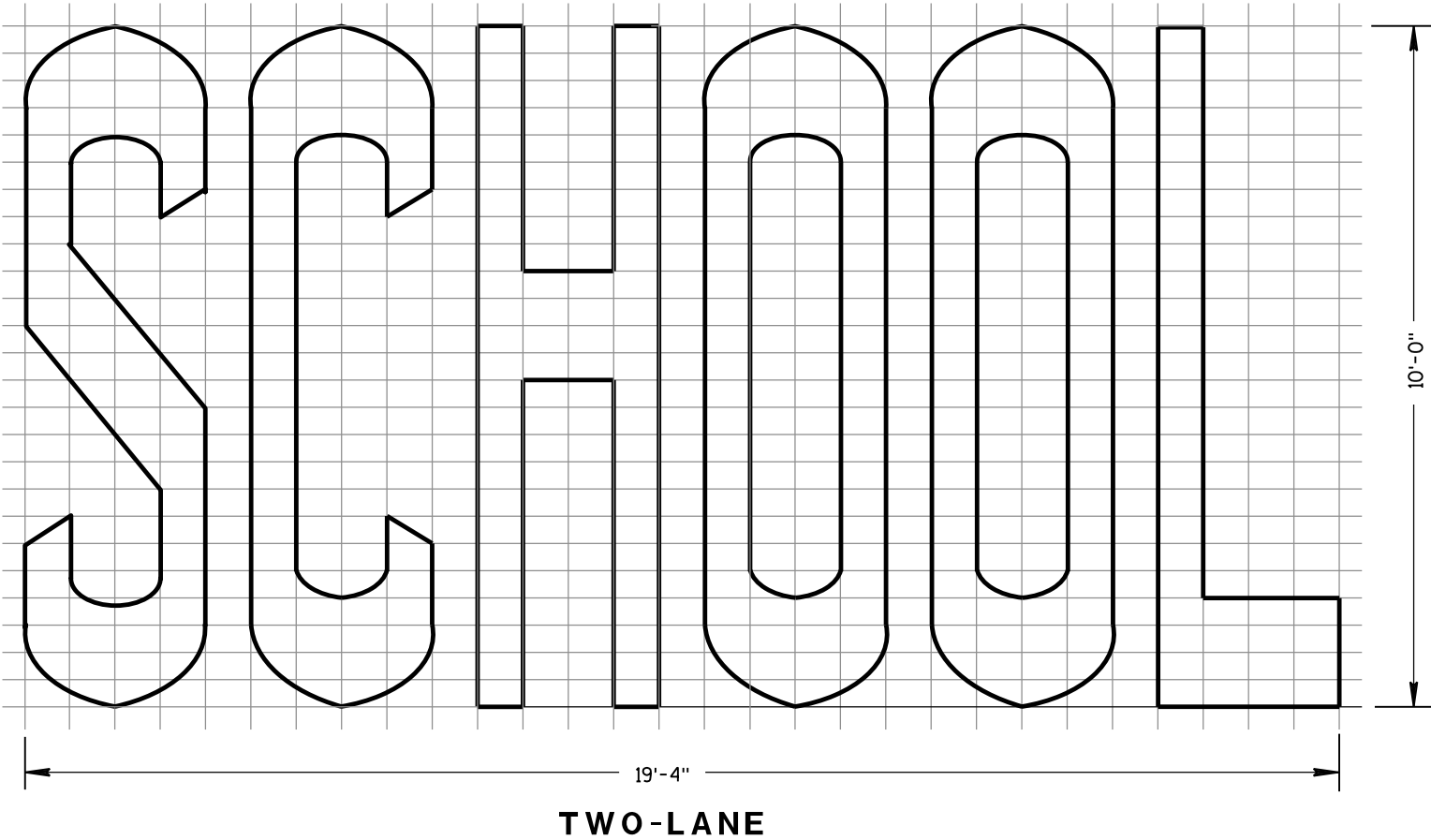
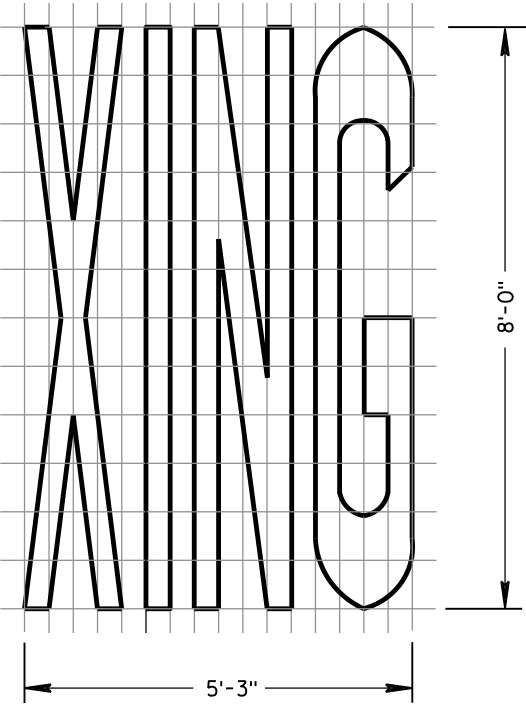
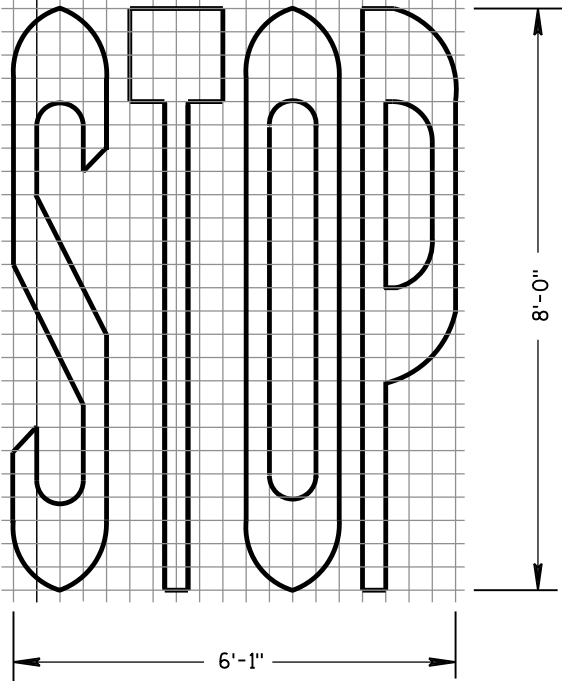
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

#### APPROVED

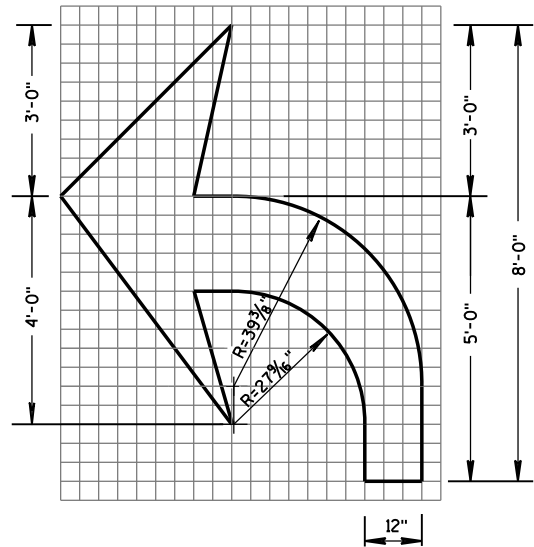
June 2017 /S/ Matthew R. Rauch  
DATE STATE SIGNING AND MARKING ENGINEER  
FHWA

GENERAL NOTES

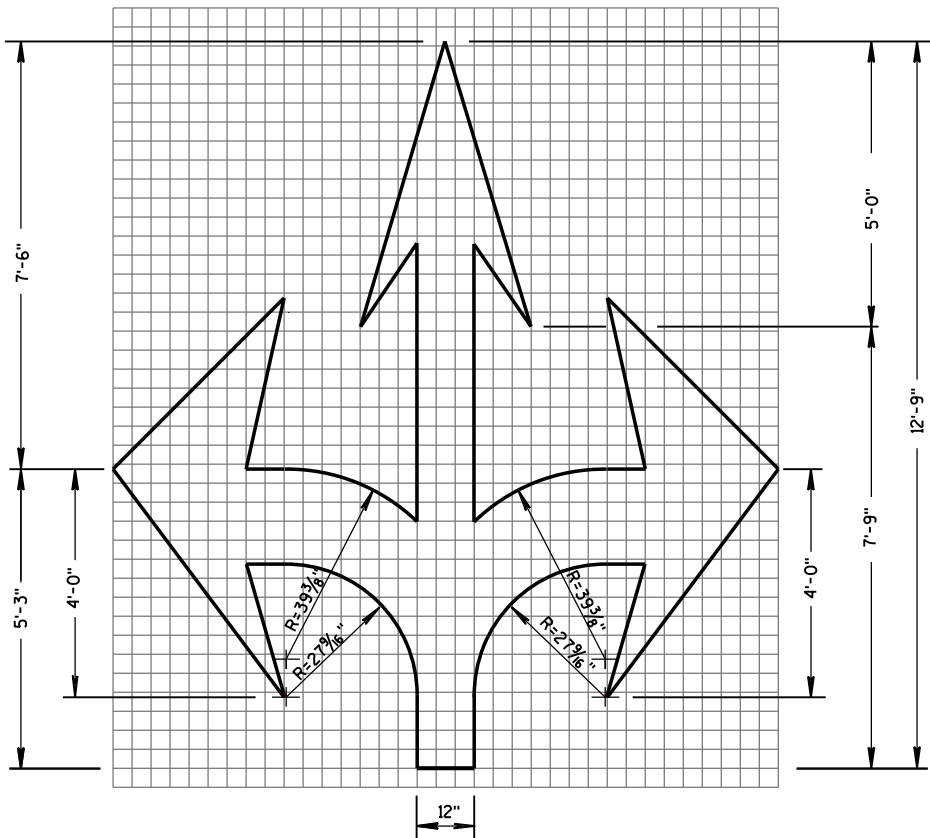
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



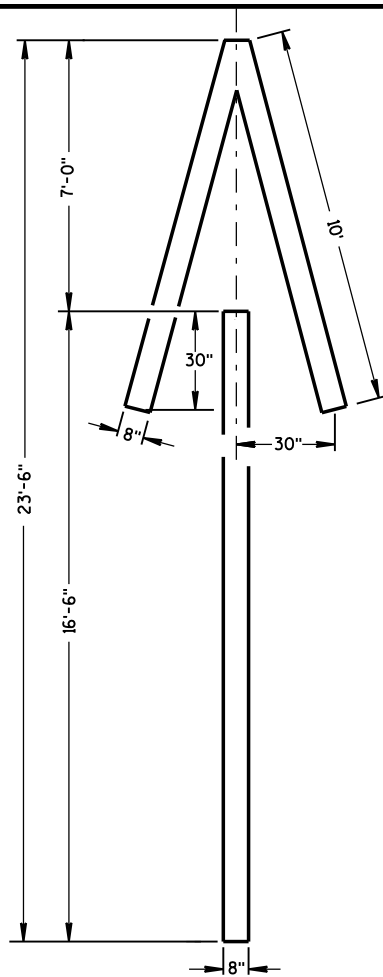
PAVEMENT MARKING WORDS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Matthew R. Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	



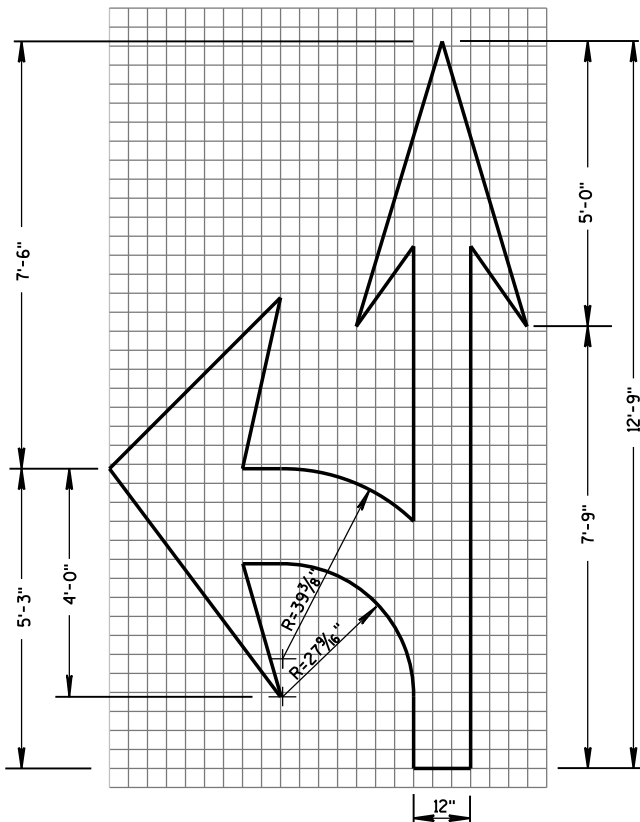
TYPE 2



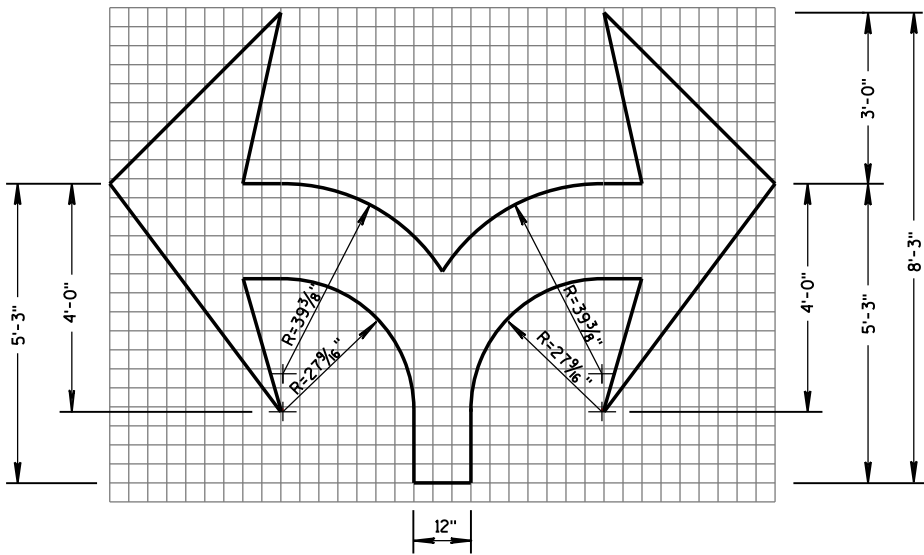
TYPE 6



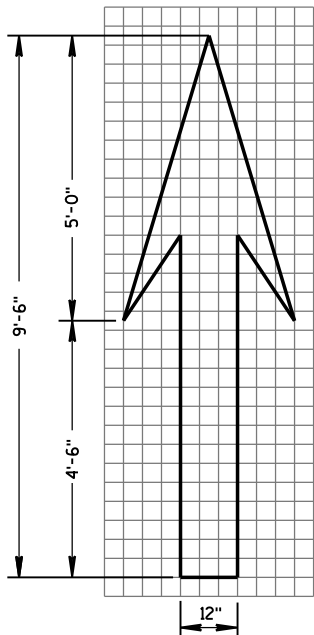
TYPE 4



TYPE 3



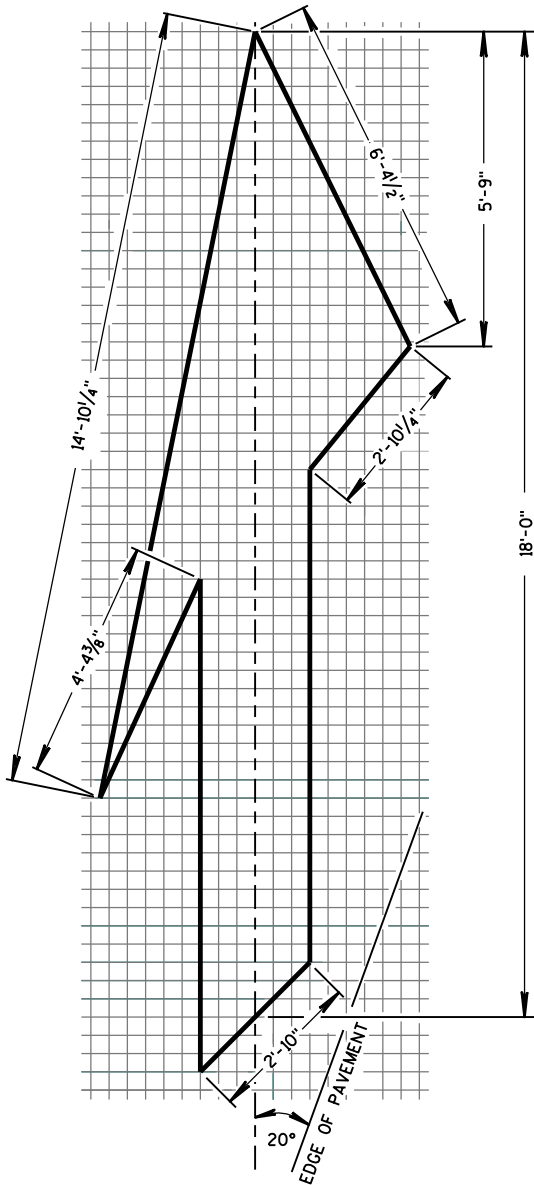
TYPE 7



TYPE 1

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

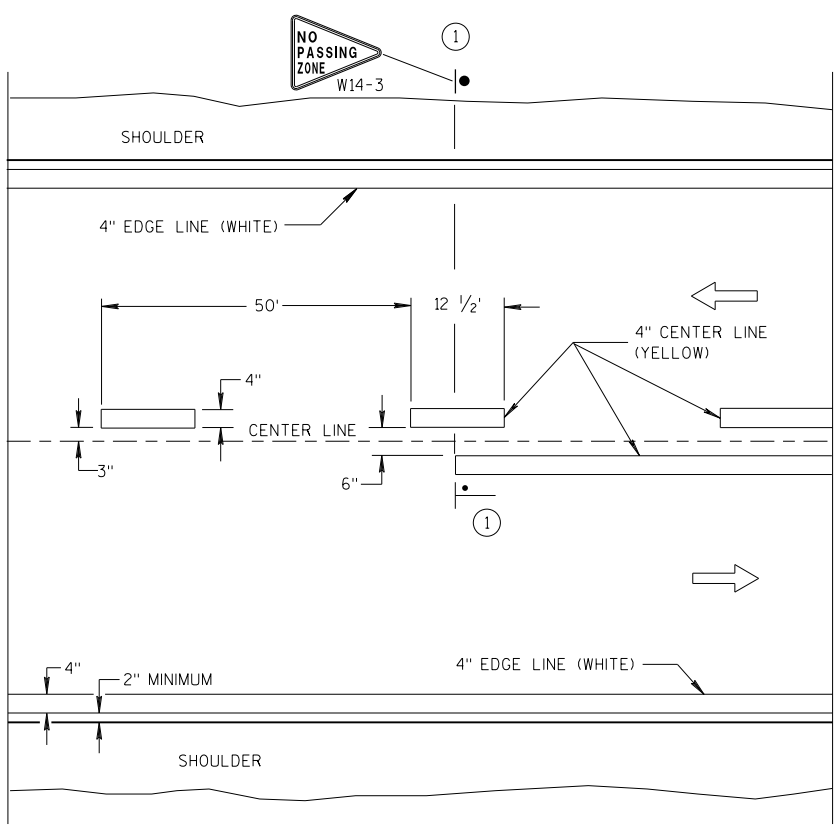


TYPE 5 LANE DROP ARROW

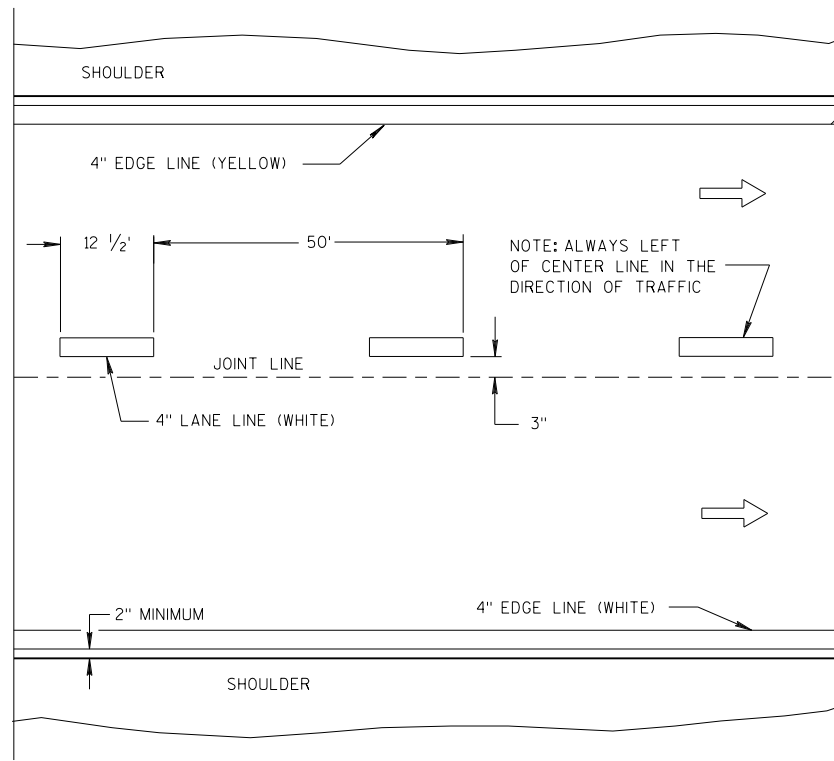
PAVEMENT MARKING ARROWS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
June 2017 /S/ Matthew R. Rauch  
DATE STATE SIGNING AND MARKING ENGINEER  
FHWA

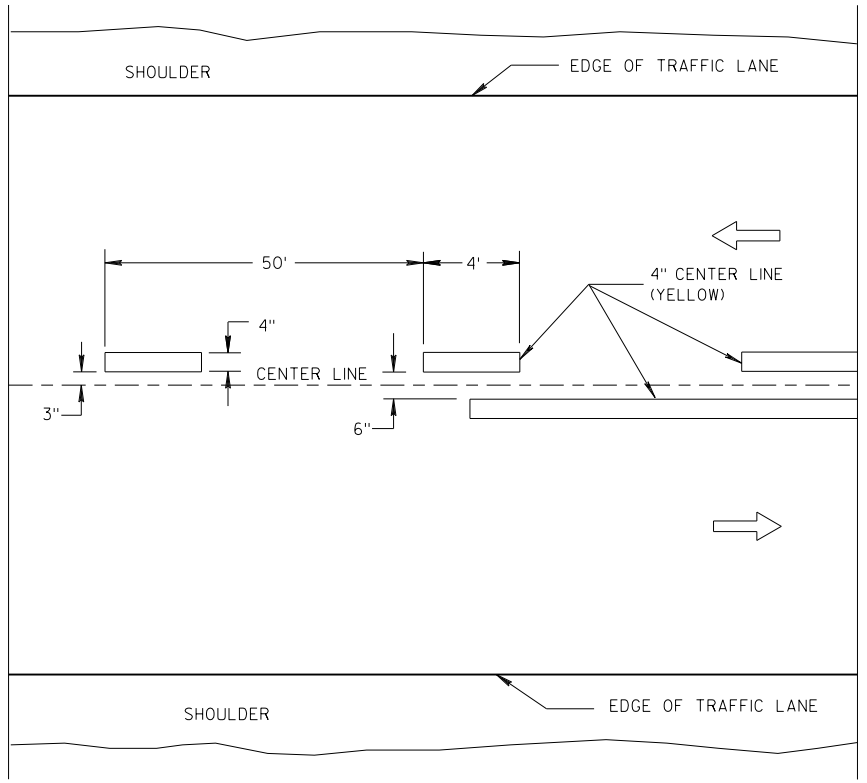


TWO WAY TRAFFIC

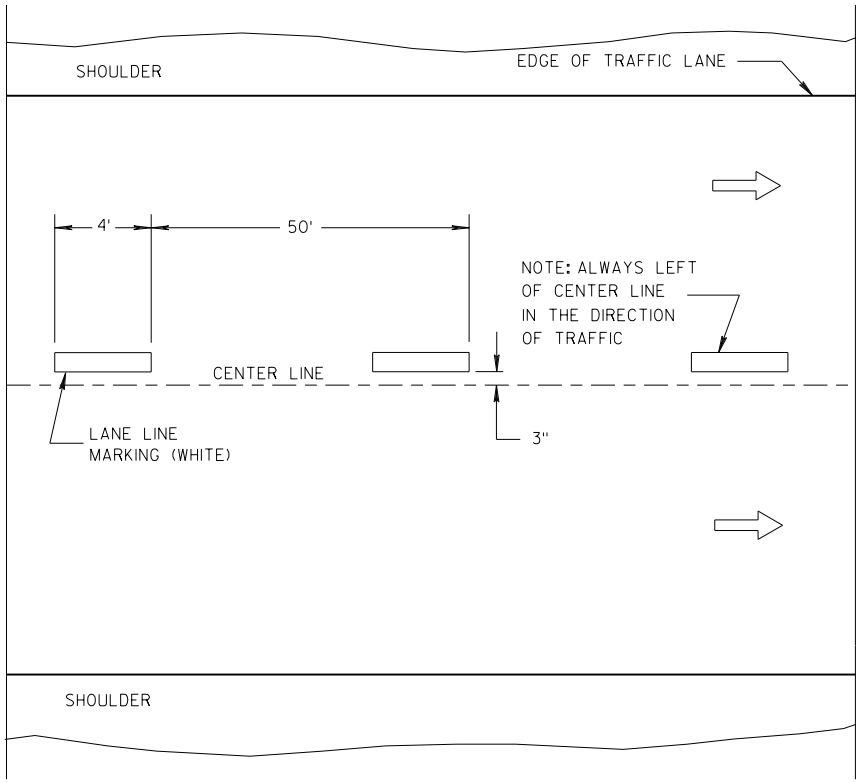


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (➡) SHOWS DIRECTION OF TRAVEL

LEGEND

- "T" MARKING
- POST MOUNTED SIGN

LONGITUDINAL MARKING (MAINLINE)

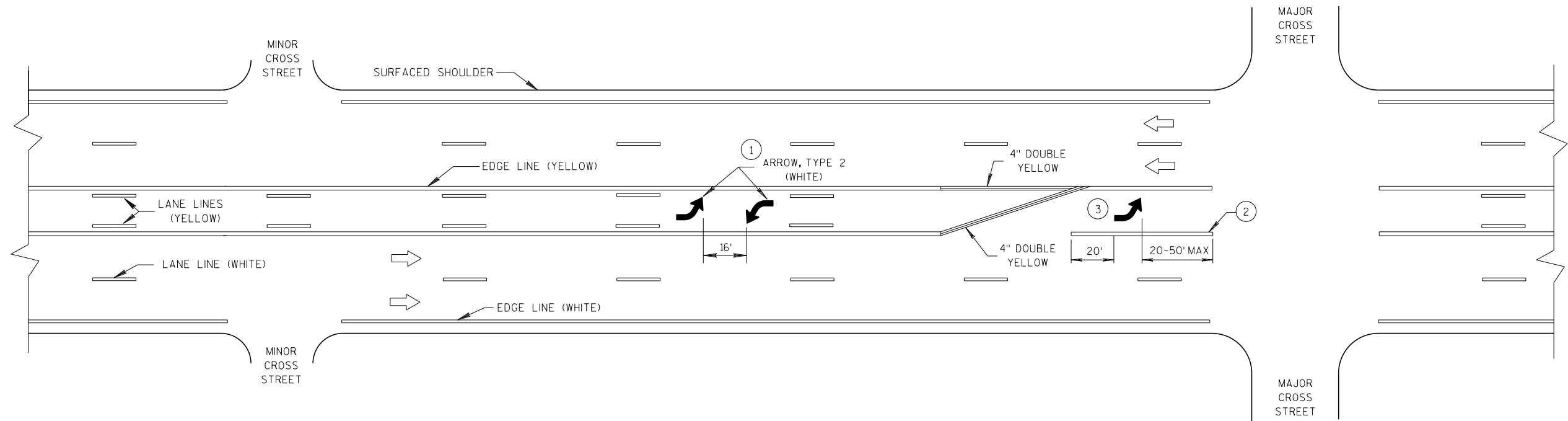
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
7/2018 /S/ Matthew R. Rauch  
DATE STATE SIGNING AND MARKING ENGINEER  
FHWA

GENERAL NOTES

- 1 A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- 2 8" WHITE
- 3 TURN BAY LENGTH OF LESS THAN 48'DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT

DIRECTION OF TRAFFIC



TWO WAY LEFT TURN LANE

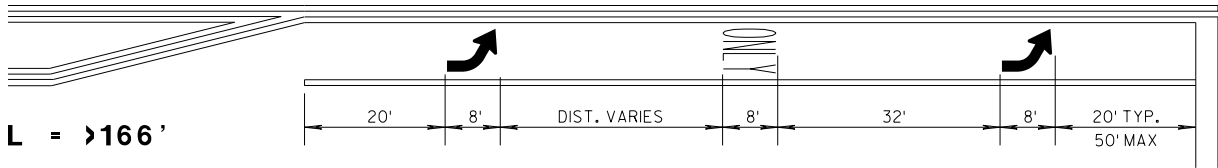
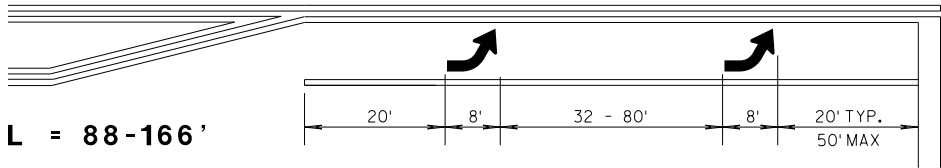
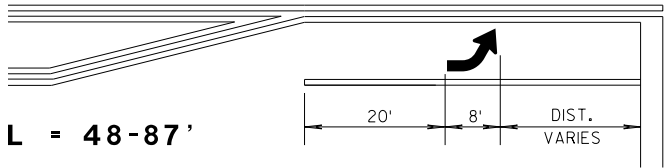
PAVEMENT MARKING  
(TURN LANES)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



TURN LANE OPTIONS

LENGTH OF TURN BAY (L) OF 0-47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



\*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

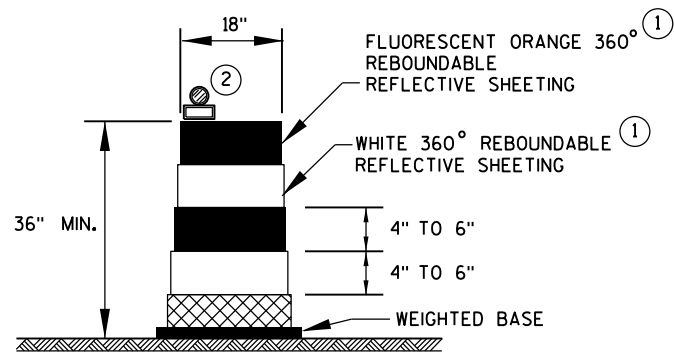
GENERAL NOTES

- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROW ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION, THE ARROWS AND ONLY MARKING ARE ELIMINATED.

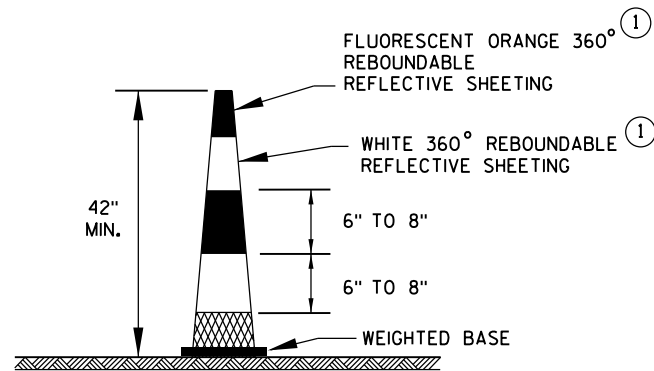
→ DIRECTION OF TRAFFIC  
L = LENGTH OF TURN BAY

PAVEMENT MARKING  
(TURN LANES)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**DRUM**

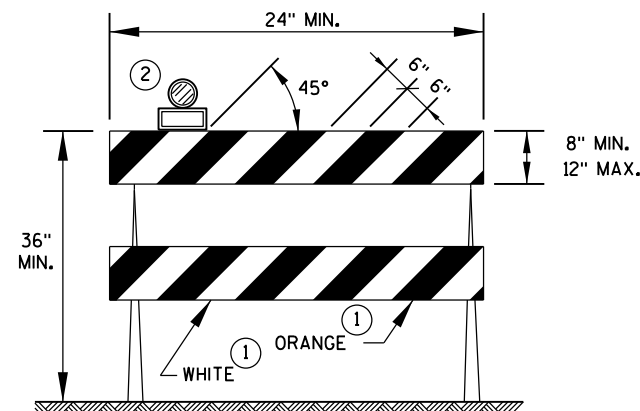


**42" CONE**

DO NOT USE IN TAPERS  
1/2 SPACING OF DRUMS

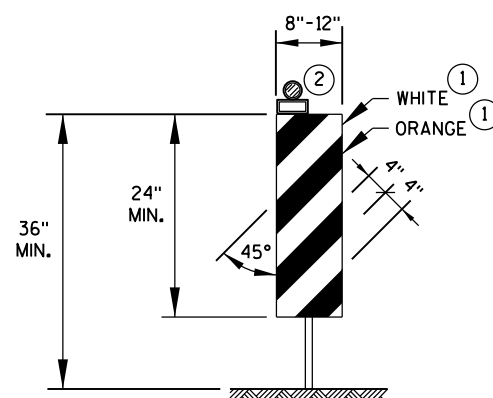
**GENERAL NOTES**

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



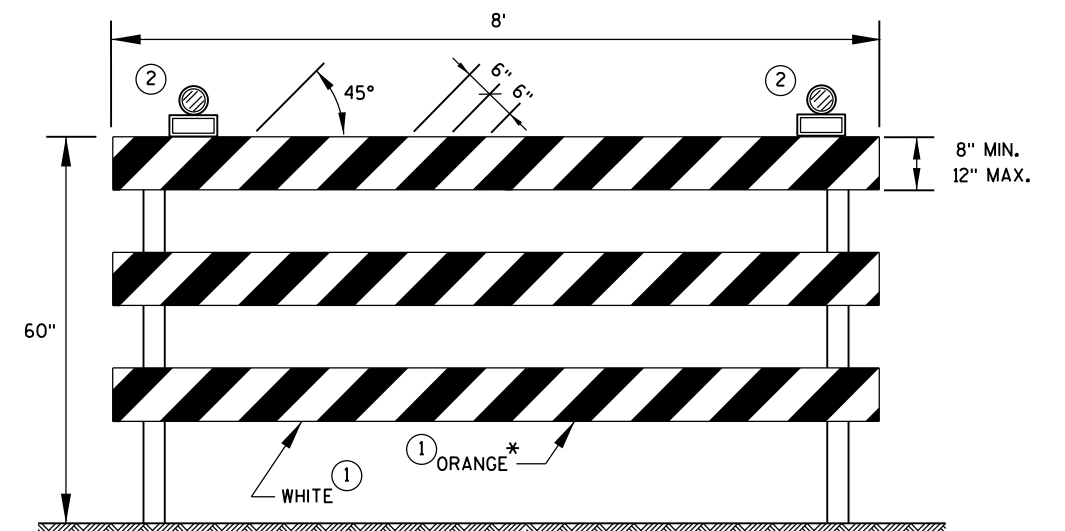
**TYPE 2 BARRICADE**

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.  
ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



**VERTICAL PANEL**

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE 3 BARRICADE**

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

CHANNELIZING DEVICES  
DRUMS, CONES, BARRICADES  
AND VERTICAL PANELS

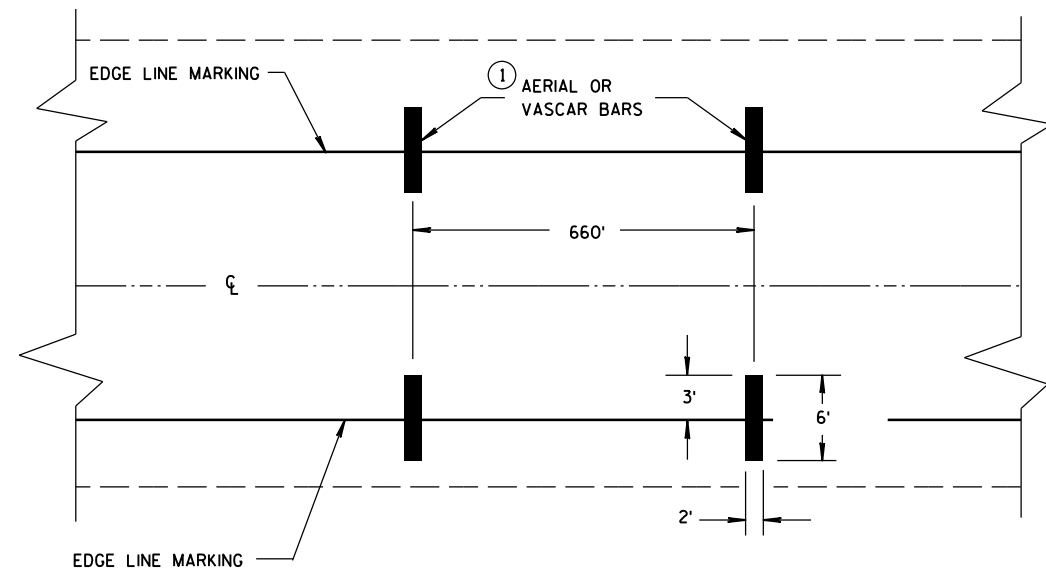
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

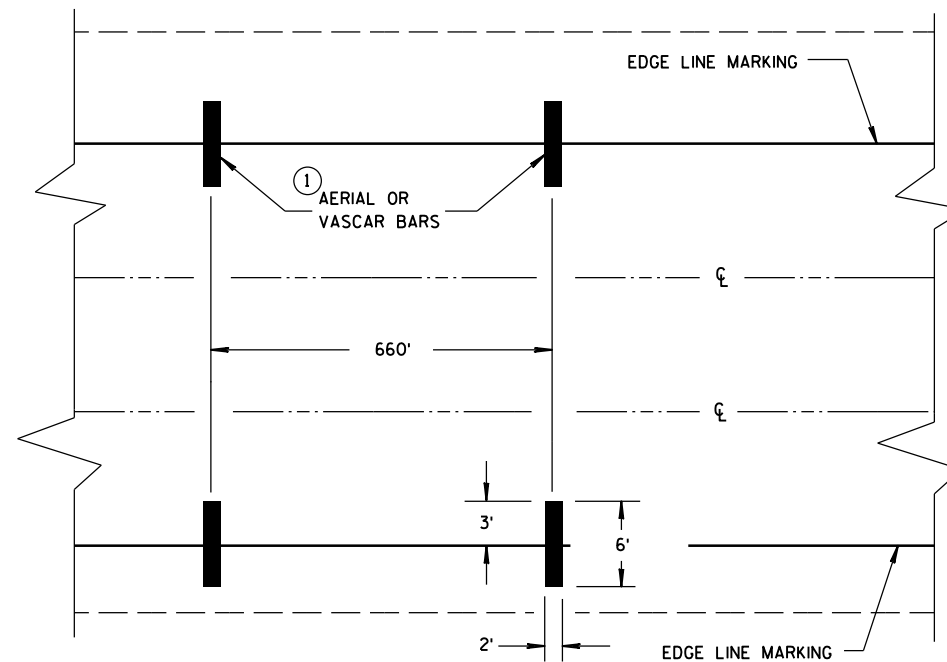
June 2017  
DATE

FHWA

/S/ Andrew Heidtke  
WORK ZONE ENGINEER



TYPICAL FOR TWO WAY OR ONE WAY TRAFFIC



TYPICAL FOR MULTILANE TRAFFIC

## SPEED ENFORCEMENT ZONE WITH AERIAL OR VASCAR BARS

## GENERAL NOTES

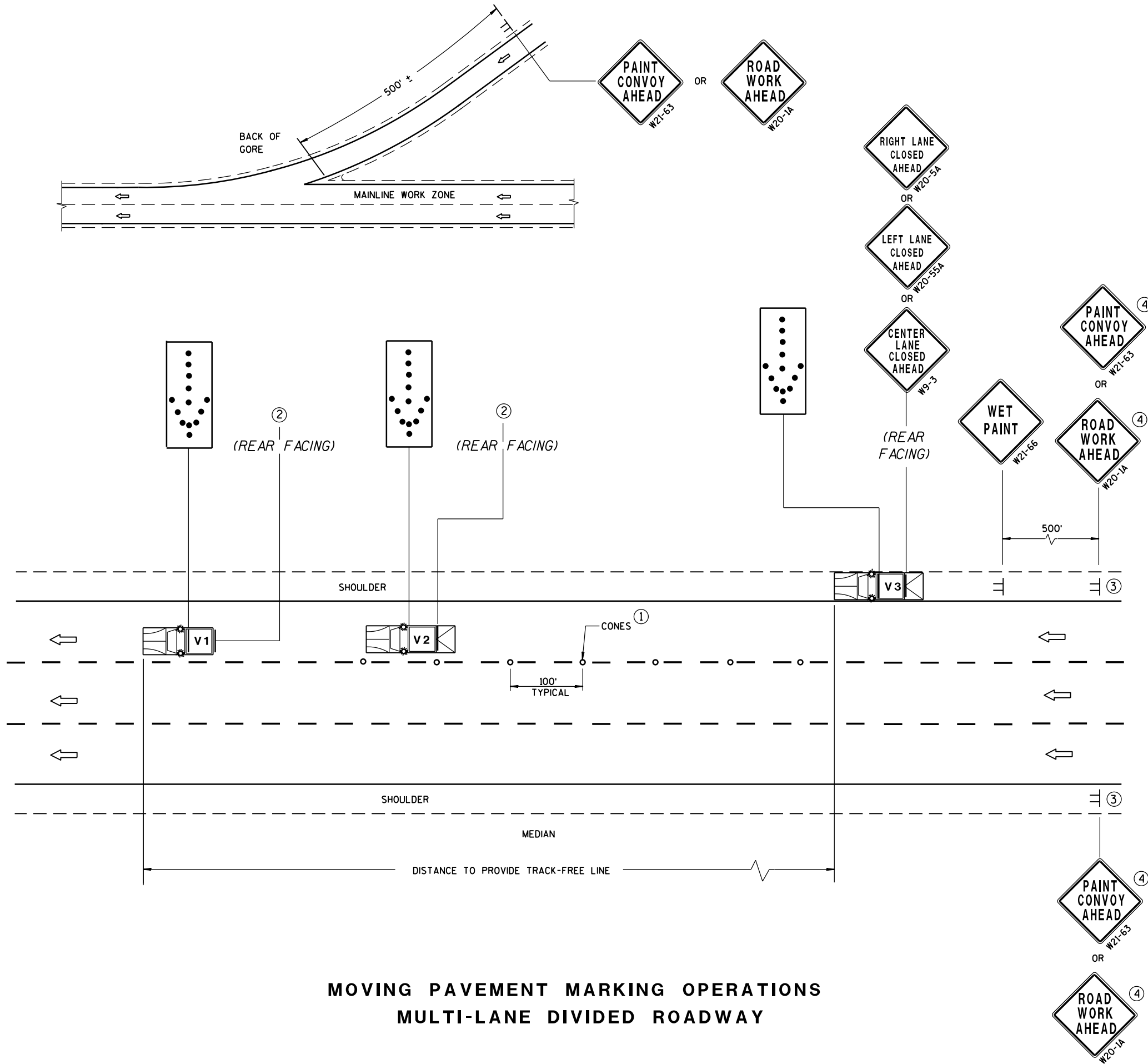
① PLACE TWO TO FIVE AERIAL OR VASCAR BARS AT 660 FOOT SPACING.

A CAR CAN BE PROVIDED BY THE WISCONSIN STATE PATROL FOR TRAFFIC CONTROL.

AERIAL ENFORCEMENT BARS  
PAVEMENT MARKING DETAILS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
June 2017 /S/ Matthew R. Rauch  
DATE STATE SIGNING AND MARKING ENGINEER  
FHWA



GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.



WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

FOR EDGE LINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH. USE DOUBLE ARROWS WHEN CONVOY IS IN CENTER LANE ONLY.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

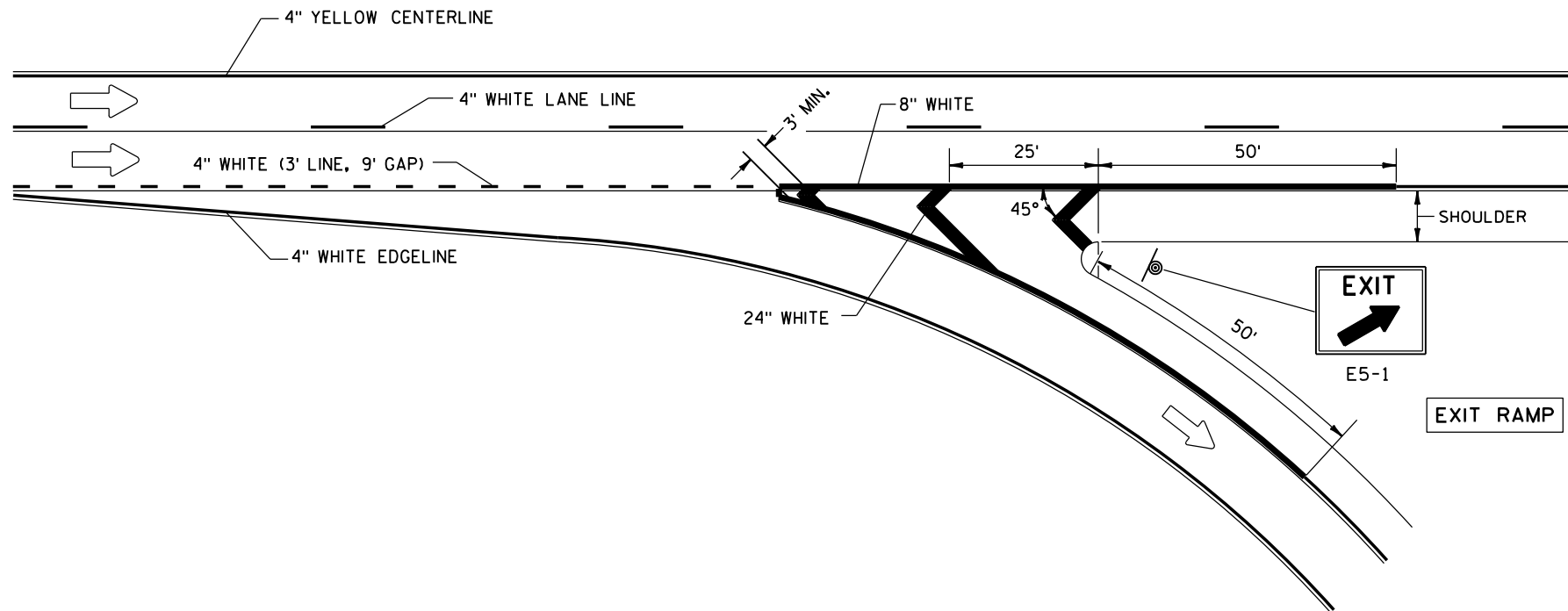
THIS DRAWING SHALL BE USED FOR EDGE LINE OR LANELINE MARKING FOR MULTILANE DIVIDED ROADWAYS.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.  
 OR   
W21-64 W21-64
- ③ SIGNS SHALL BE REPEATED AFTER EVERY ON RAMP OR EVERY THREE MILES.
- ④ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

- V1 LEAD VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE WITH TMA
- TMA TRUCK-MOUNTED ATTENUATOR
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- CONES
- FLASHING ARROW PANEL (MERGE)

MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE Sept. 2017	/S/ Andrew Heldtke WORK ZONE ENGINEER
FHWA	



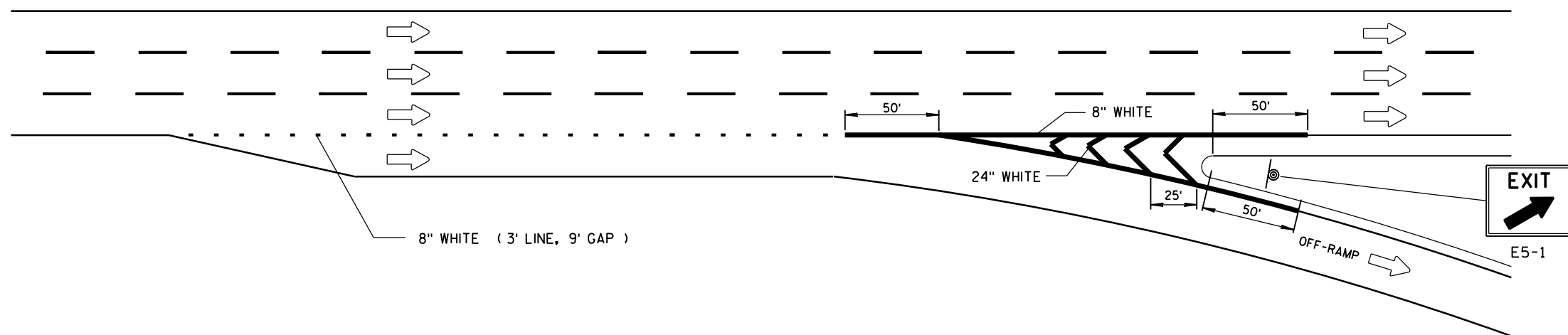
PAVEMENT MARKING FOR EXIT RAMP

## GENERAL NOTES

PLACE GROOVE 3 INCHES LEFT OF JOINT.

## LEGEND

- DIRECTION OF TRAVEL
- SIGN ON PERMANENT SUPPORT



SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL EXIT-RAMP

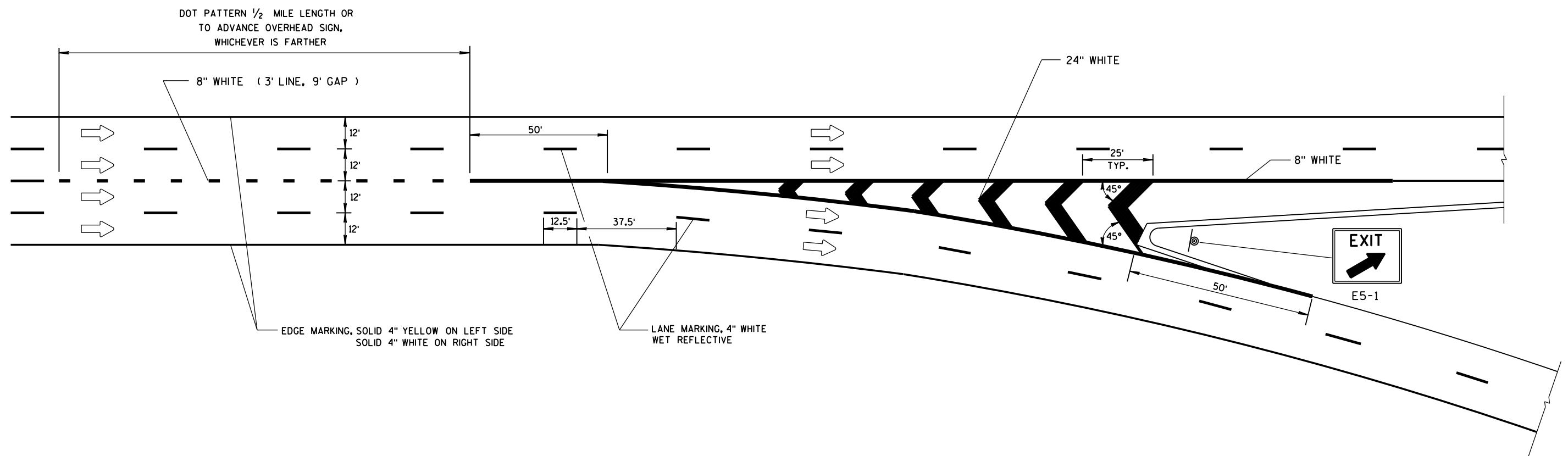
PAVEMENT MARKING  
(RAMPS AND GOES)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PLACE GROOVE 3 INCHES LEFT OF JOINT.

➡ DIRECTION OF TRAVEL

Ⓢ SIGN ON PERMANENT SUPPORT



STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**GENERAL NOTES**

PLACE GROOVE 3 INCHES LEFT OF JOINT.

① 1/2 LENGTH OF FULL WIDTH ACCELERATION LANE.

**PAVEMENT MARKING FOR ENTRANCE RAMP**

**LEGEND**

➡ DIRECTION OF TRAVEL

**SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL ENTRANCE-RAMP**

**PAVEMENT MARKING FOR  
PARALLEL ON-RAMP AND  
PARALLEL OFF-RAMP**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
Sept., 2017 /S/ Matthew R. Rauch  
DATE STATE SIGNING AND MARKING ENGINEER  
FHWA

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMENENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"W0" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

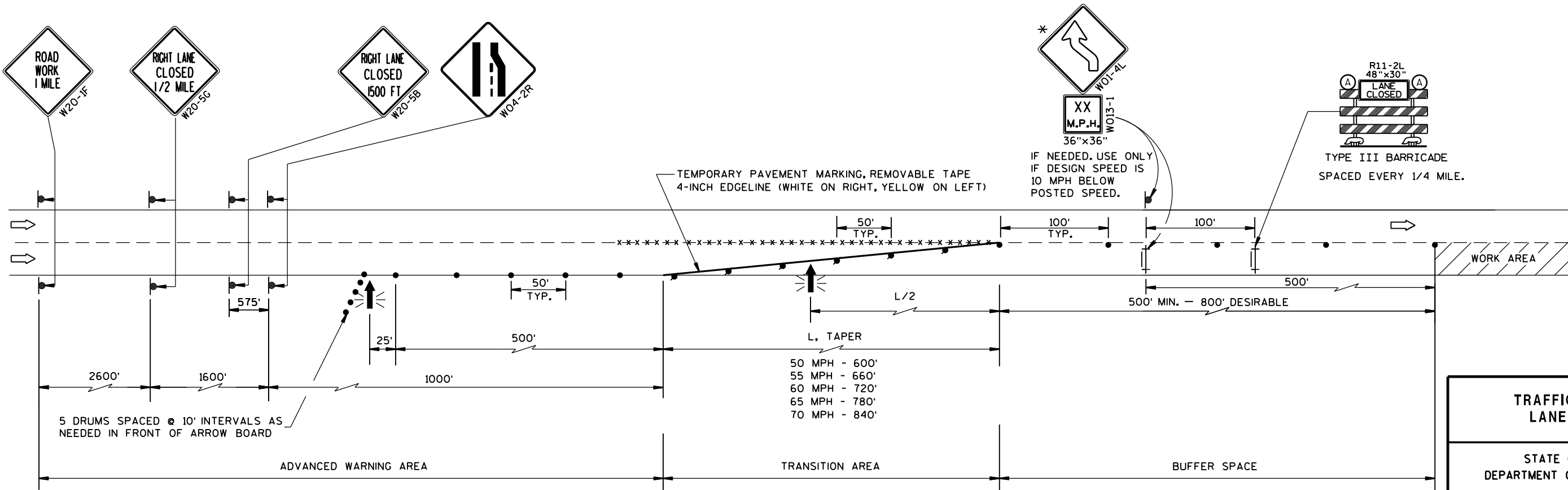
REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR LONGER THAN 4 OR MORE DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

\* THE LEFT REVERSE CURVE SIGN (W01-4L) IS ONLY REQUIRED WHEN THIS DETAIL IS USED IN COMBINATION WITH "SINGLE LANE CROSSOVER" DETAIL.



TRAFFIC CONTROL, LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March 2016 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR LONGER THAN 4 OR MORE DAYS AND NIGHTS.

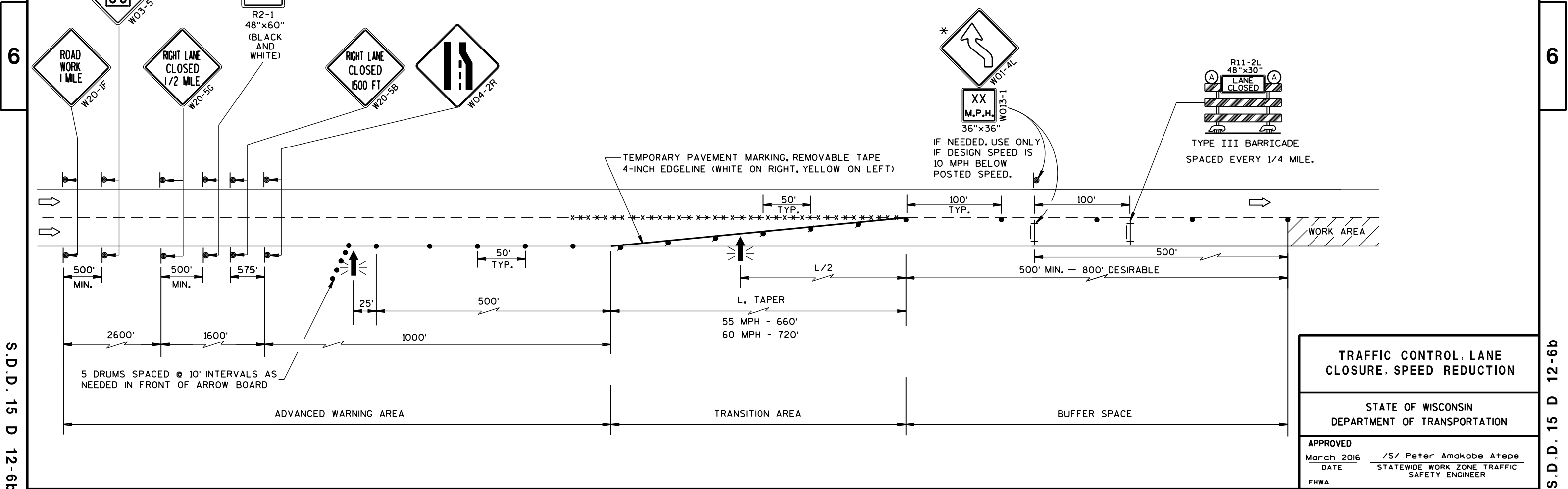
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

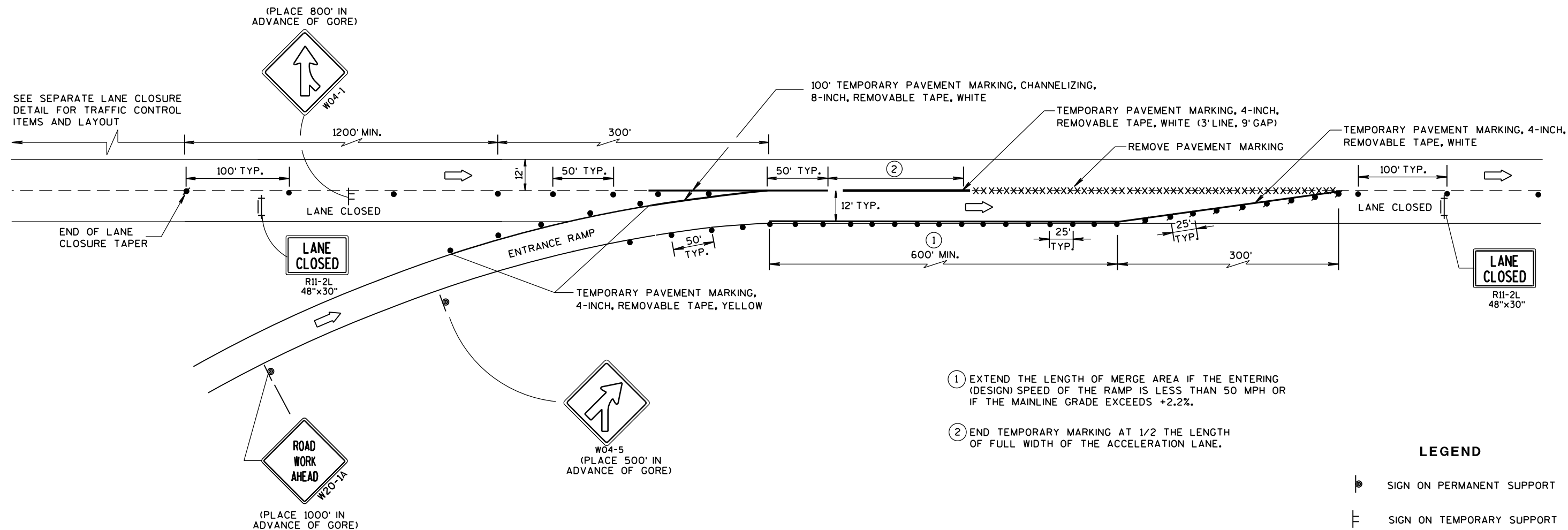
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

\* THE LEFT REVERSE CURVE SIGN (WO1-4L) IS ONLY REQUIRED WHEN THIS DETAIL IS USED IN COMBINATION WITH "SINGLE LANE CROSSOVER" DETAIL.

\*\* A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. THERE SHOULD BE A SPEED LIMIT SIGN INCORPORATED A MINIMUM OF EVERY 2 OR 3 MILES. INCLUDE A RESUME SPEED LIMIT SIGN 200 FEET MINIMUM (500 FEET DESIREABLE) BEYOND THE "END OF ROADWORK" SIGN.



TRAFFIC CONTROL, ENTRANCE RAMP WITHIN RIGHT LANE CLOSURE



PARALLEL ENTRANCE RAMP

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

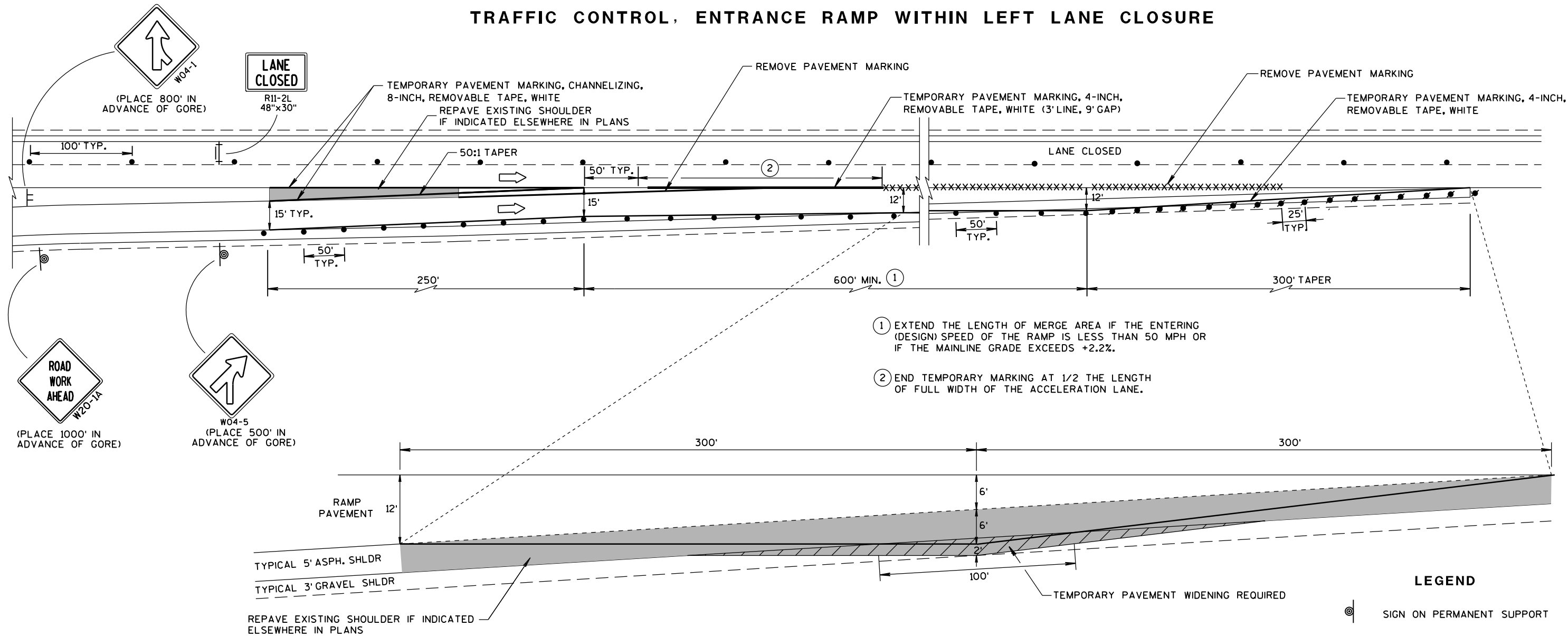
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL, PARALLEL ENTRANCE RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED Sept., 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LEFT LANE CLOSURE



TEMPORARY PAVEMENT DETAIL

(EXISTING RAMP DIMENSIONS MAY VARY. ADJUST TEMPORARY PAVEMENT ACCORDINGLY)

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"W0" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

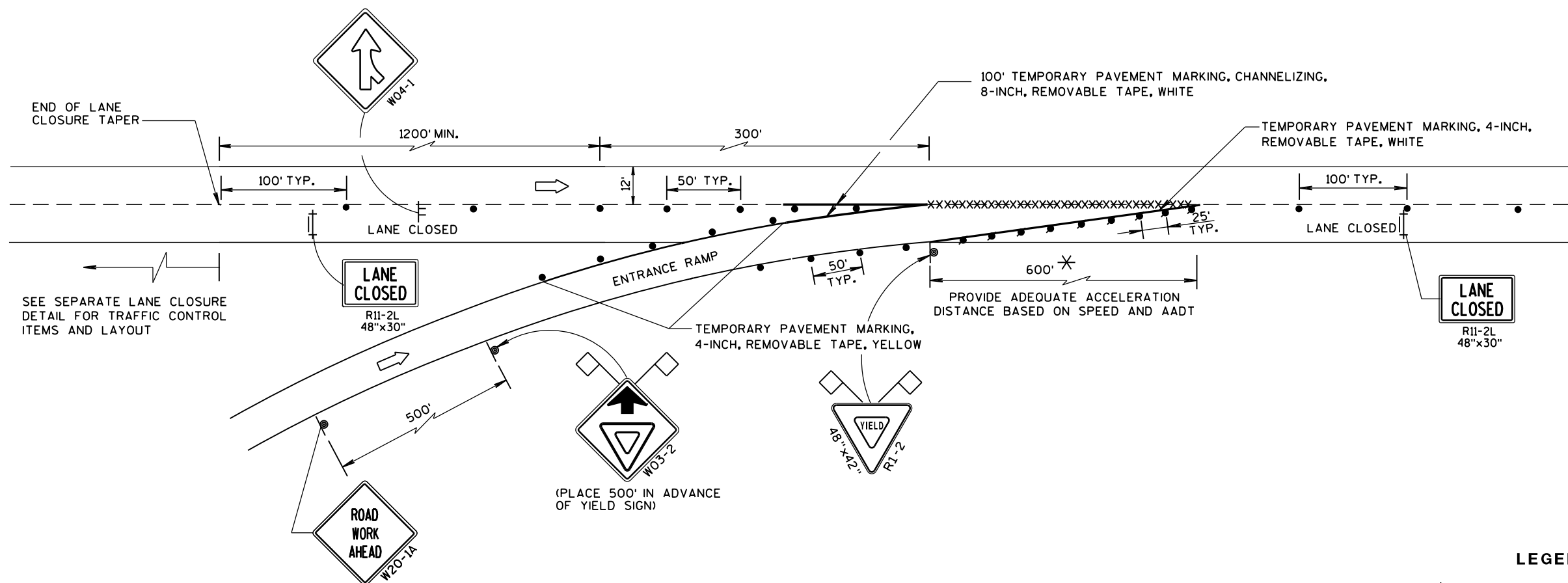
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL, ENTRANCE RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED Sept., 2017 DATE	/S/ Andrew Heldtke WORK ZONE ENGINEER
FHWA	



**TAPERED ENTRANCE RAMP  
WITHIN RIGHT LANE CLOSURE**

**GENERAL NOTES**

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

✱ CONSULT WITH REGIONAL WORK ZONE ENGINEER IF NEED TO REDUCE LENGTH EXISTS.

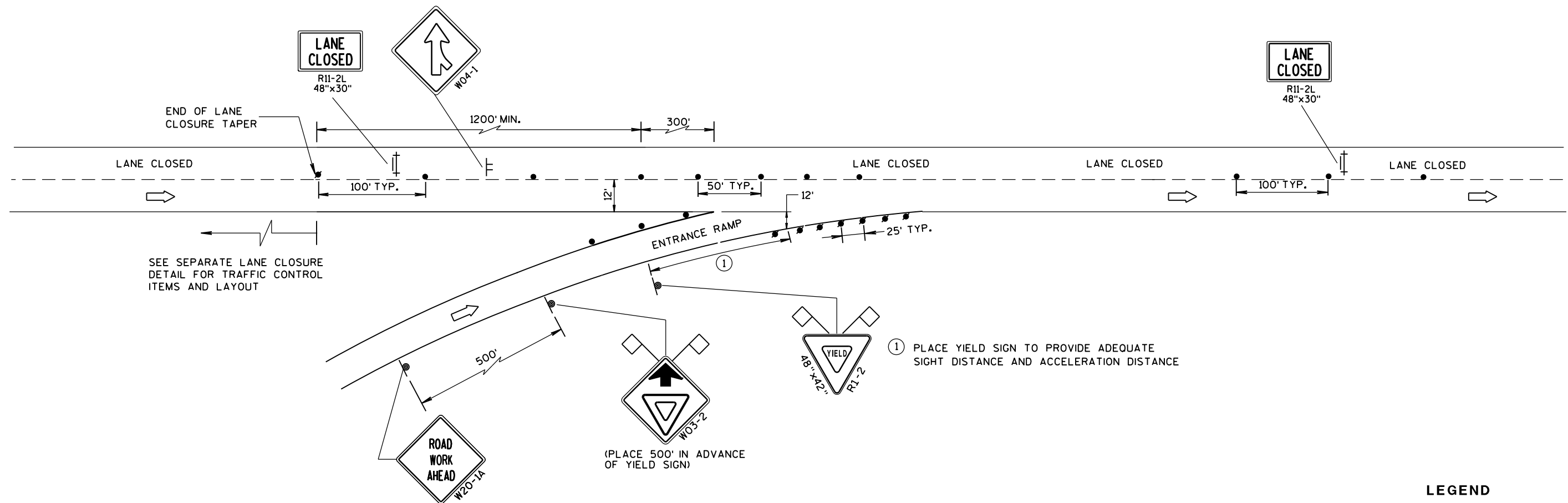
**LEGEND**

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- TYPE III BARRICADE WITH ATTACHED SIGN
- FLAGS, 16" x 16" MIN., (ORANGE)
- DIRECTION OF TRAFFIC

**TRAFFIC CONTROL,  
TAPERED ENTRANCE RAMP  
WITHIN LANE CLOSURE**

**STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION**

APPROVED  
Sept., 2017 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA



## TAPERED ENTRANCE RAMP WITHIN LEFT LANE CLOSURE

### GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

### LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- TYPE III BARRICADE WITH ATTACHED SIGN
- FLAGS, 16" x 16" MIN., (ORANGE)
- DIRECTION OF TRAFFIC

### TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

Sept., 2017

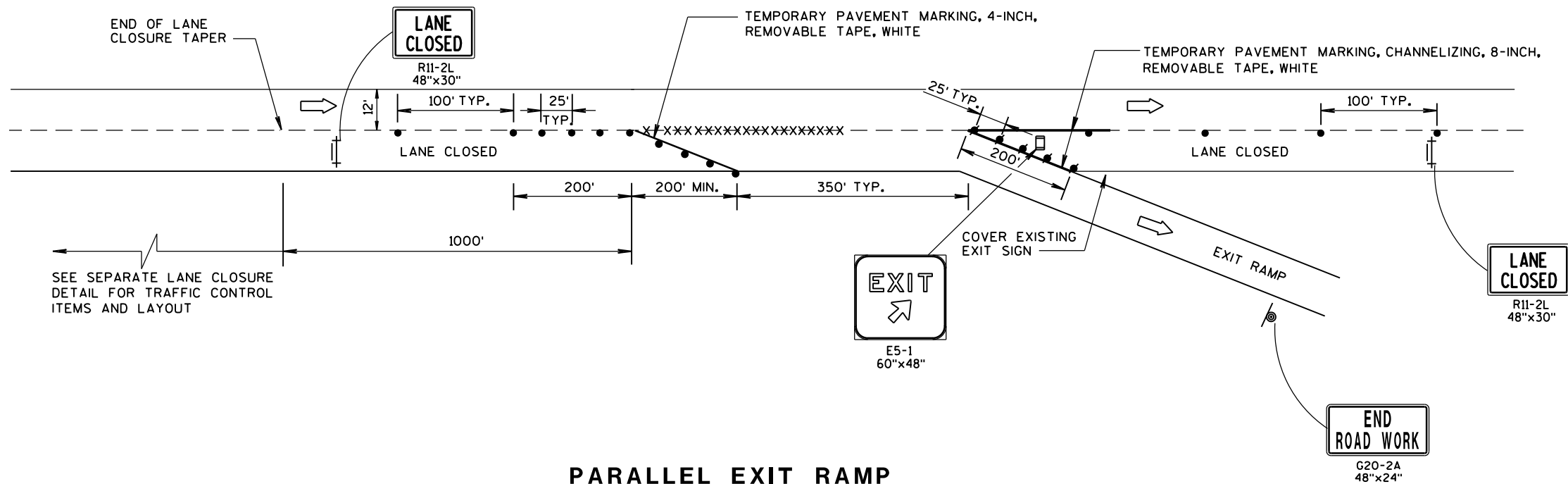
DATE

FHWA

/S/ Andrew Heidtke

WORK ZONE ENGINEER





PARALLEL EXIT RAMP

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP, AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"W0" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- TYPE III BARRICADE WITH ATTACHED SIGN
- FLAGS, 16" x 16" MIN., (ORANGE)
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,  
PARALLEL EXIT RAMP  
WITHIN LANE CLOSURE

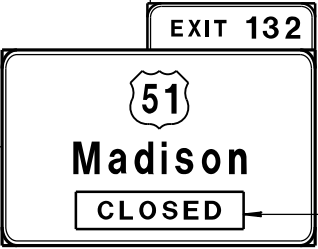
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: Sept., 2017 /S/ Andrew Heidtke  
WORK ZONE ENGINEER  
FHWA



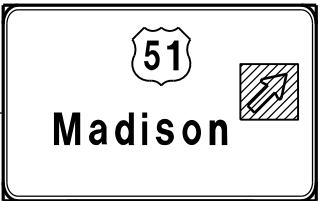
G20-60  
108"x24"

OR



G20-60  
108"x24"

PLACE SIGN G20-60 OVER MILEAGE  
ON EXISTING E1-1A SIGN



COVER ARROW ON  
EXISTING E4-1A  
SIGN (COVERING  
SIGNS TYPE I)

G20-61  
120"x30"

# GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF RAMP CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROPOFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN THE WORK IS NOT IN PROGRESS.

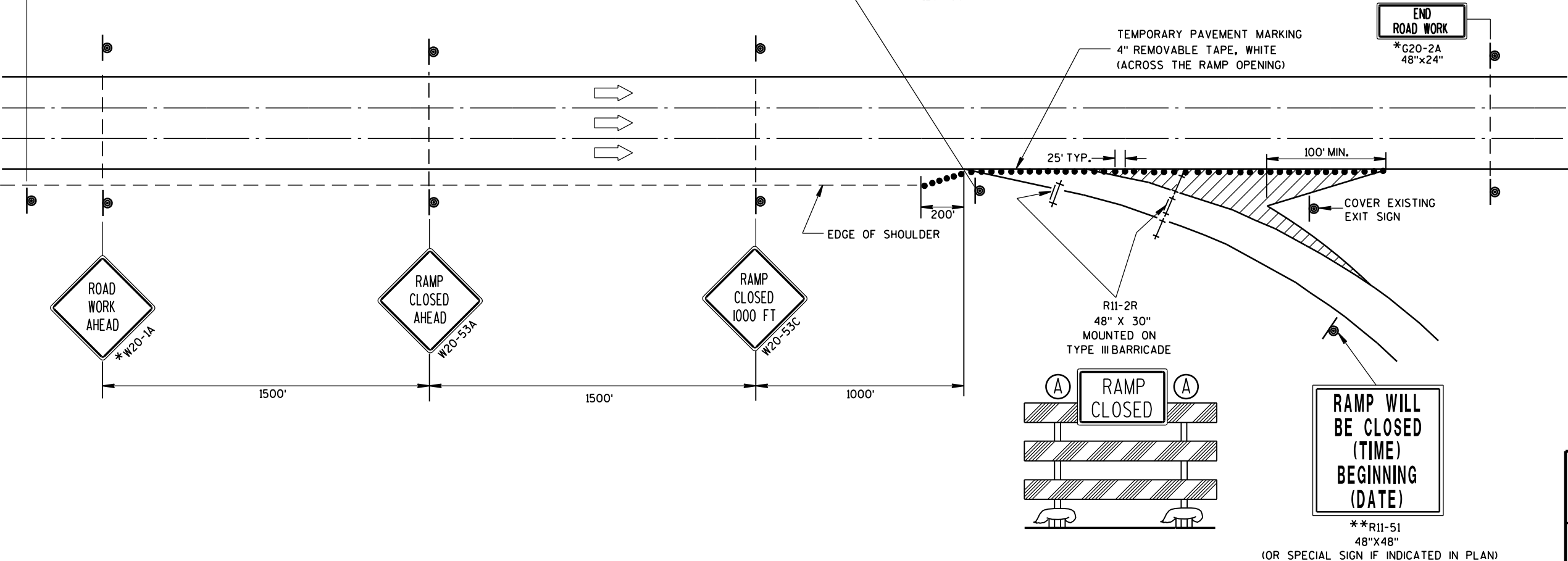
WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12-HOUR DURATION.

\* W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

\*\* PLACE "RAMP WILL BE CLOSED" SIGN 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.

6

6



## LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC

## TRAFFIC CONTROL, EXIT RAMP CLOSURE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
Sept. 2015 /S/ Peter Amakobe Atepe  
DATE STATEWIDE WORK ZONE TRAFFIC  
FHWA SAFETY ENGINEER

S.D.D. 15 D 16-3

S.D.D. 15 D 16-3

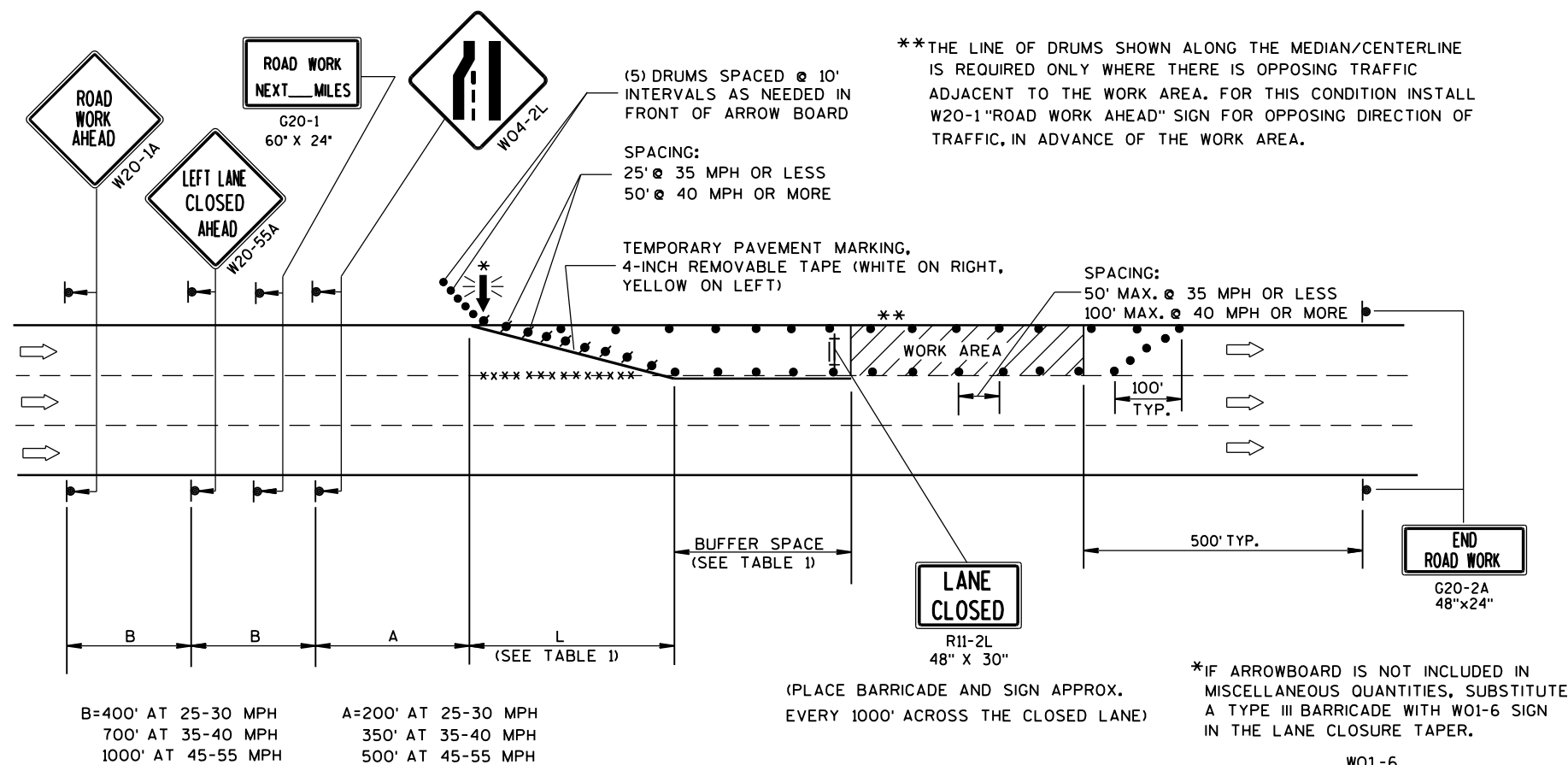


TABLE 1  
TAPER AND BUFFER SPACE  
FOR 12' LANE WIDTH

S	L	BUFFER SPACE
25	125'	55'
30	180'	85'
35	245'	120'
40	320'	170'
45	540'	220'
50	600'	280'
55	660'	335'

FOR LANE WIDTH OTHER THAN 12':

L = WS AT 45 MPH OR GREATER

L =  $\frac{WS^2}{60}$  AT 40 MPH OR LESS

L = TAPER LENGTH IN FEET

S = NON-CONSTRUCTION SPEED LIMIT (MPH)

W = WIDTH OF LANE CLOSURE

## GENERAL NOTES

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL,  
SINGLE LANE CLOSURE,  
NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2016

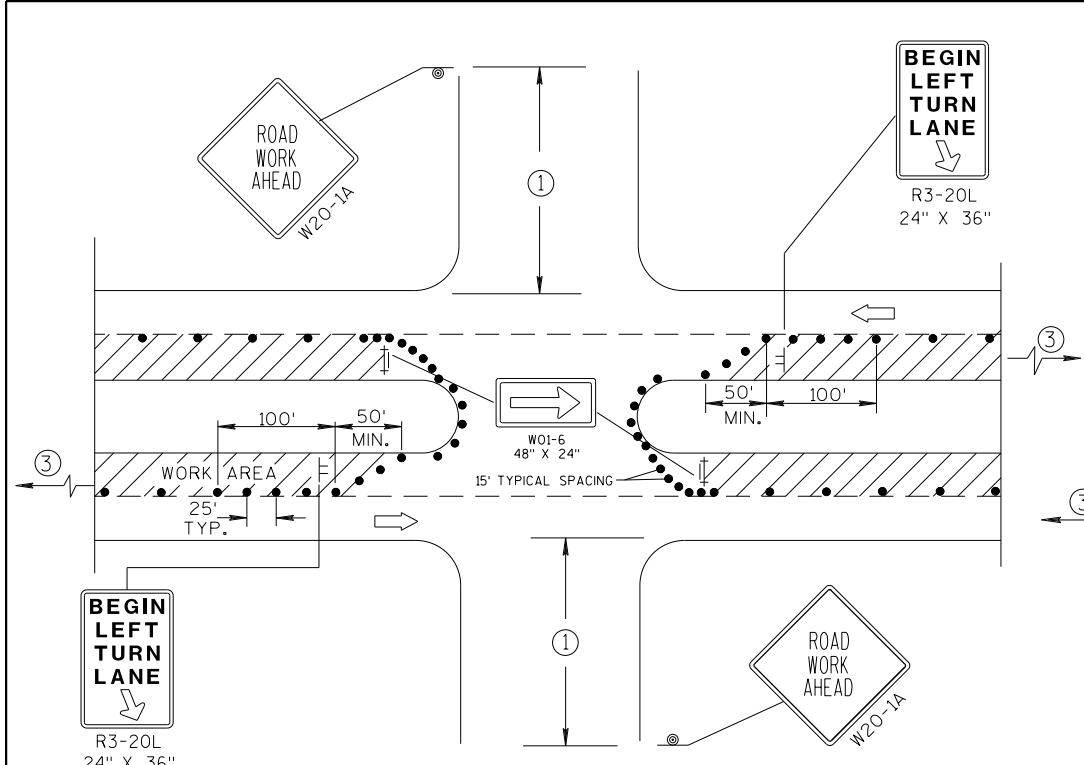
DATE

FHWA

/S/ Peter Amakobe Atepe

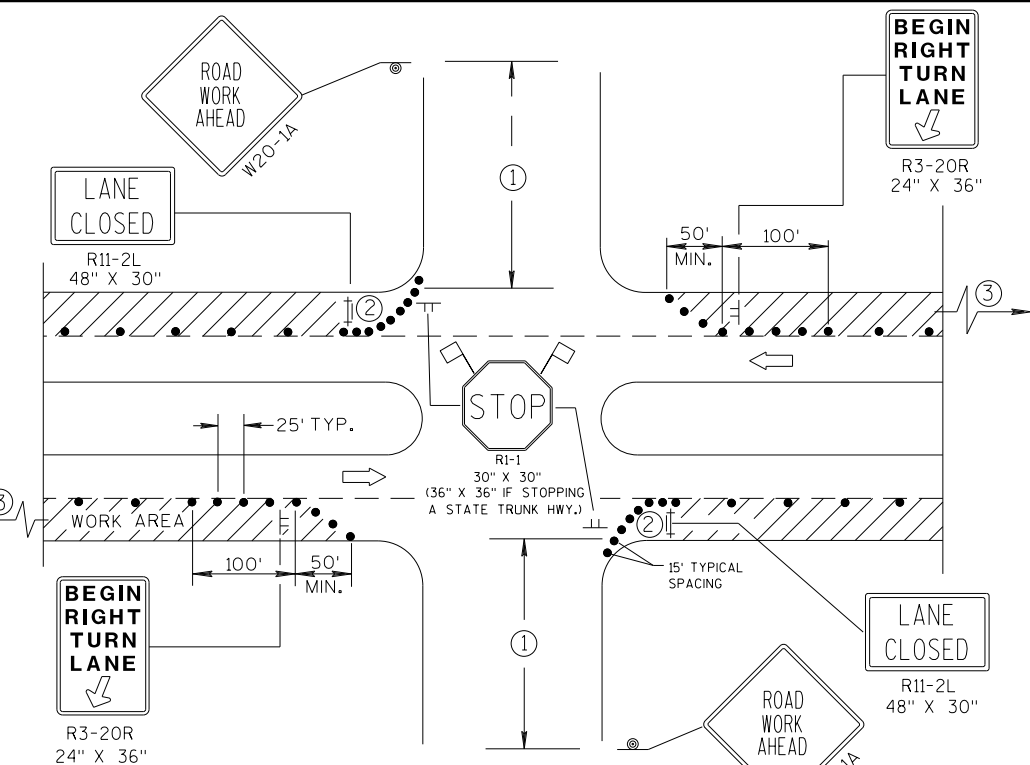
STATEWIDE WORK ZONE TRAFFIC

SAFETY ENGINEER



DETAIL A  
FOR LEFT LANE CLOSURE AT  
INTERSECTION OR MEDIAN OPENING

PROVIDE TURN LANES AT  
INTERSECTIONS WHENEVER  
STAGING OF WORK ALLOWS.  
TAPER AND TURN LANE  
LENGTHS BASED ON FIELD  
CONDITIONS AS APPROVED  
BY THE ENGINEER.



DETAIL B  
FOR RIGHT LANE CLOSURE  
AT INTERSECTION

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

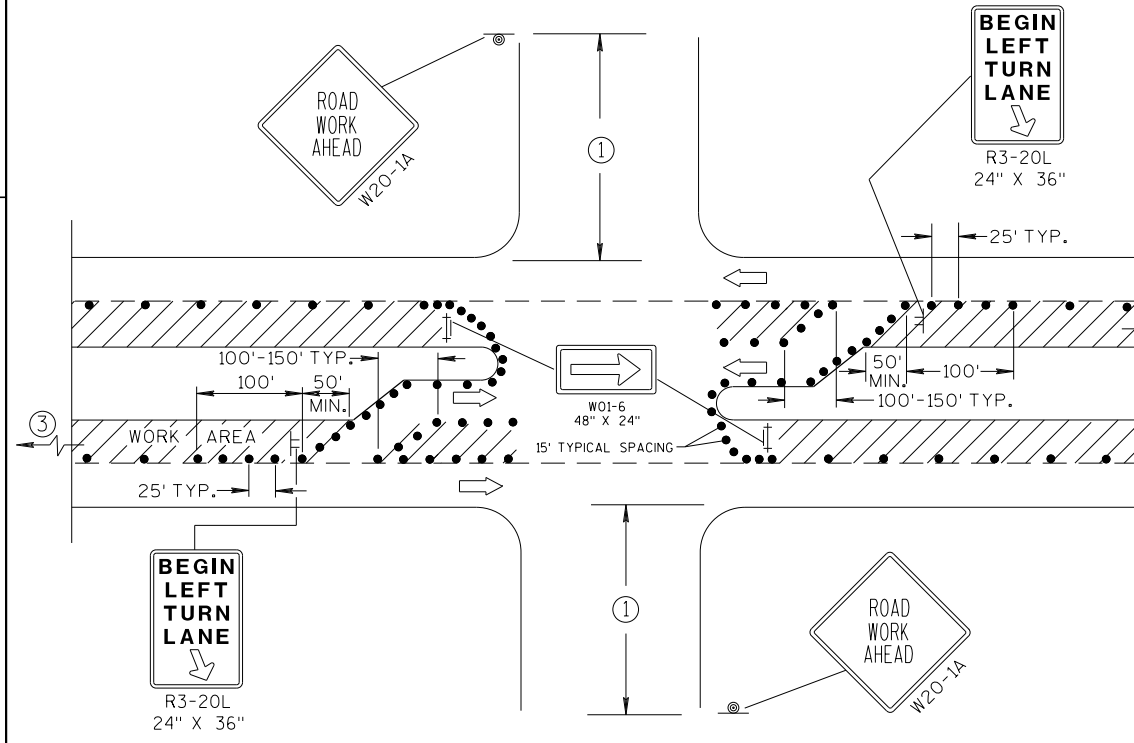
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

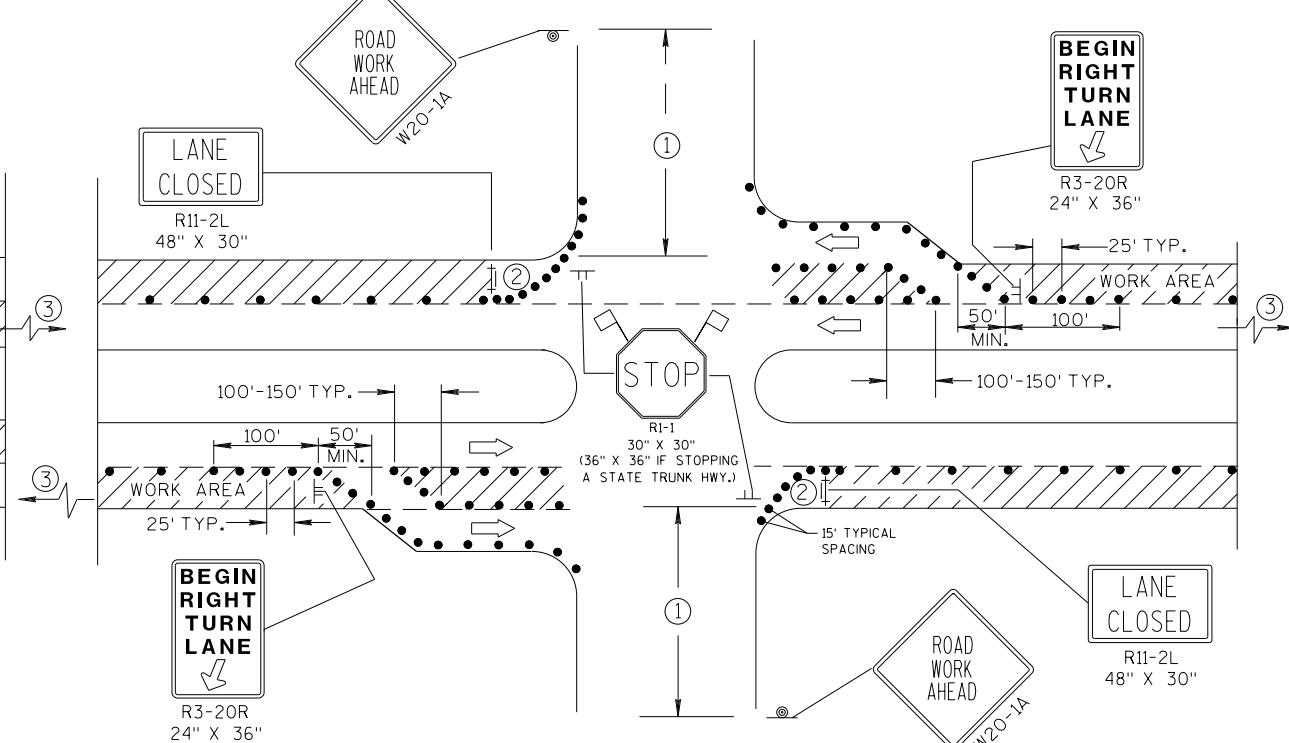
- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.  
350' IF 35-40 MPH.  
200' IF 25-30 MPH.
- ② ALSO USE BARRICADE AND 15-FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS.
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.

LEGEND

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ≡ SIGN ON TEMPORARY SUPPORT (5' MIN. MOUNTING HEIGHT)
- ≡ TYPE III BARRICADE WITH ATTACHED SIGN AND TYPE "A" WARNING LIGHT (FLASHING)
- ➡ DIRECTION OF TRAFFIC
- 🚩 FLAGS, 16" X 16" MIN., (ORANGE)
- ▨ WORK AREA



DETAIL C  
FOR LEFT LANE CLOSURE AT INTERSECTION OR  
MEDIAN OPENING (WITH LEFT TURN BAY OPEN)



DETAIL D  
FOR RIGHT LANE CLOSURE AT INTERSECTION  
(WITH RIGHT TURN BAY OPEN)

TRAFFIC CONTROL,  
INTERSECTION WITHIN  
SINGLE LANE CLOSURE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

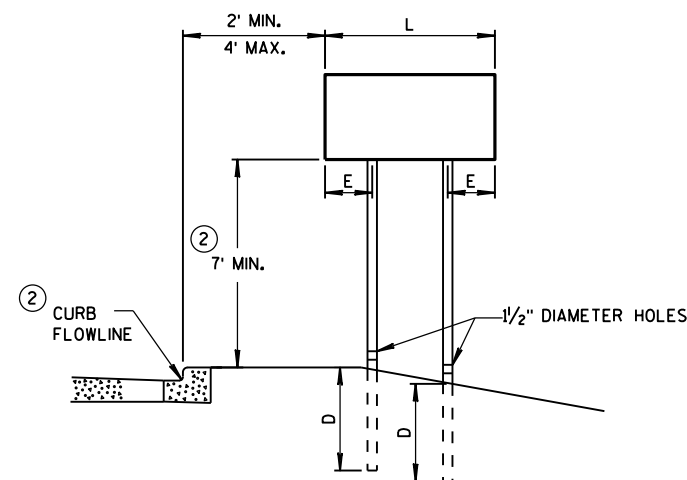
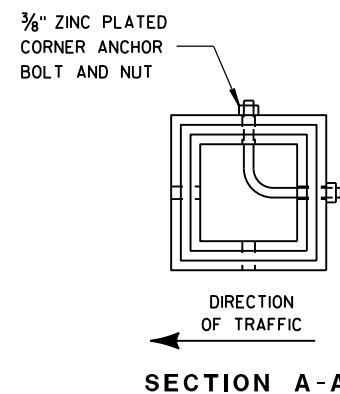
APPROVED  
7/2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA



## TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS LARGER THAN 27 SQ.FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

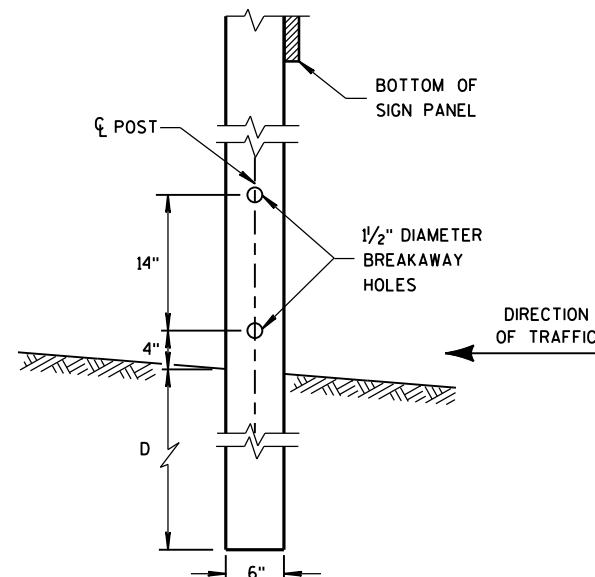


**URBAN AREA**

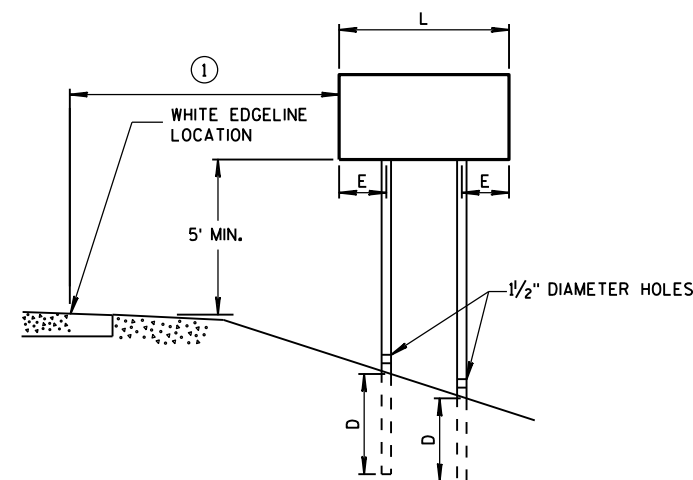
## POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST  
EMBEDMENT DEPTH

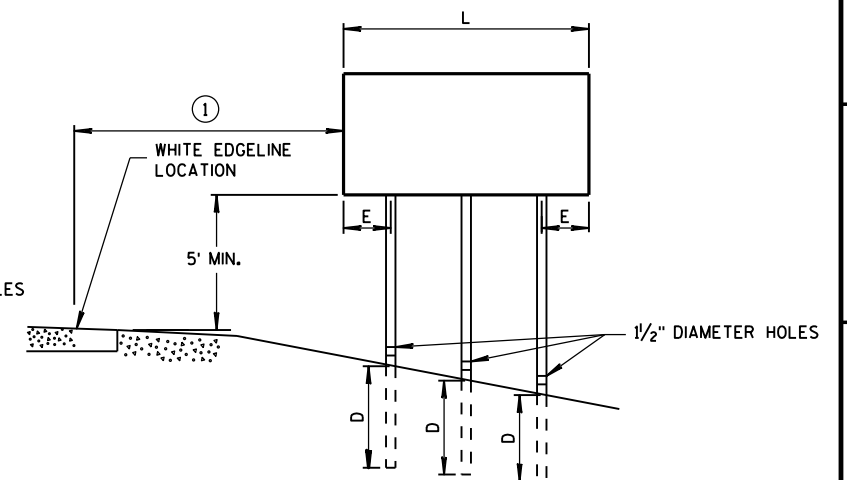
AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



## 4" x 6" WOOD POST MODIFICATION



## RURAL AREA



## GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

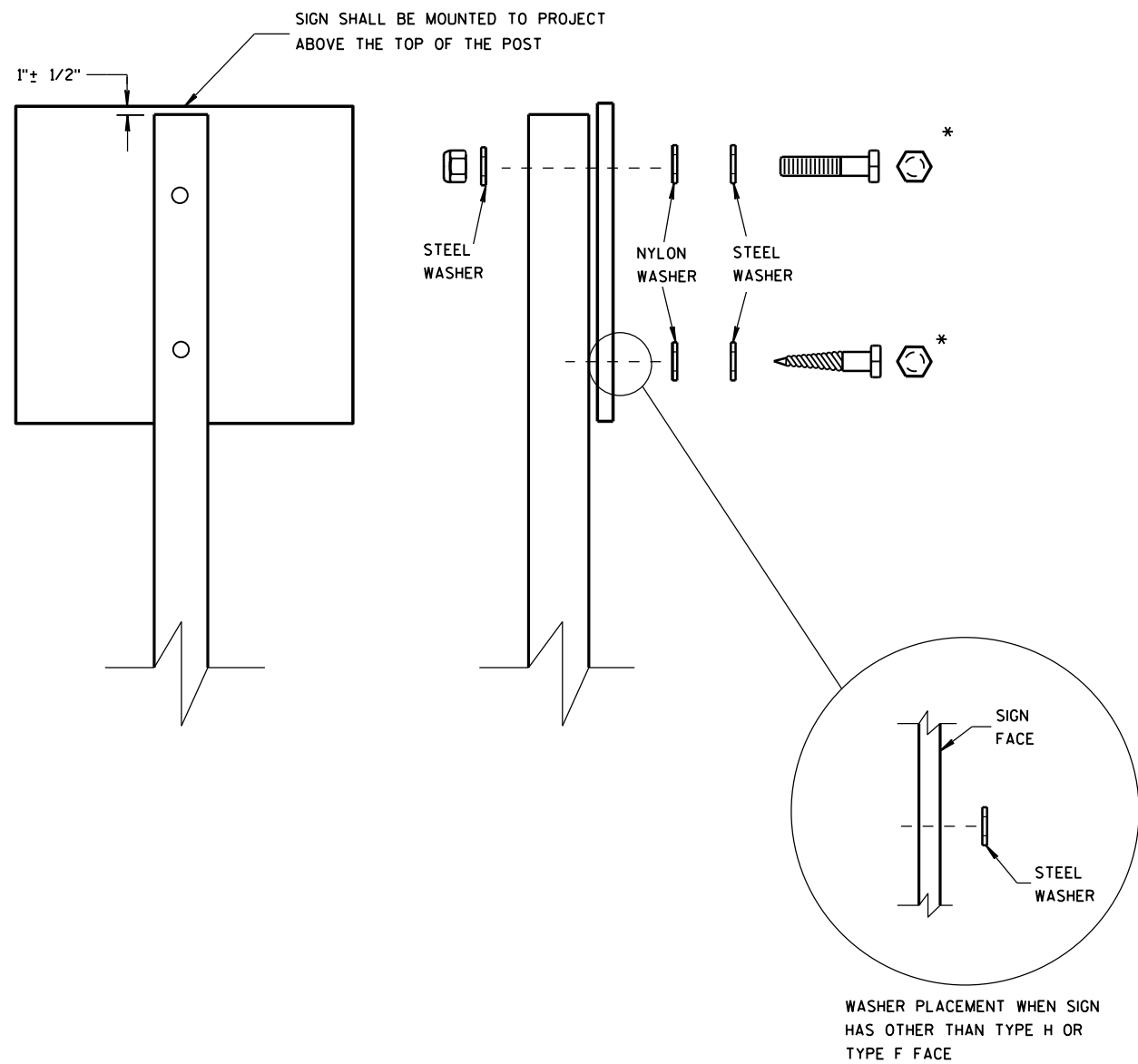
4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

SEE NOTE (3)

## TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

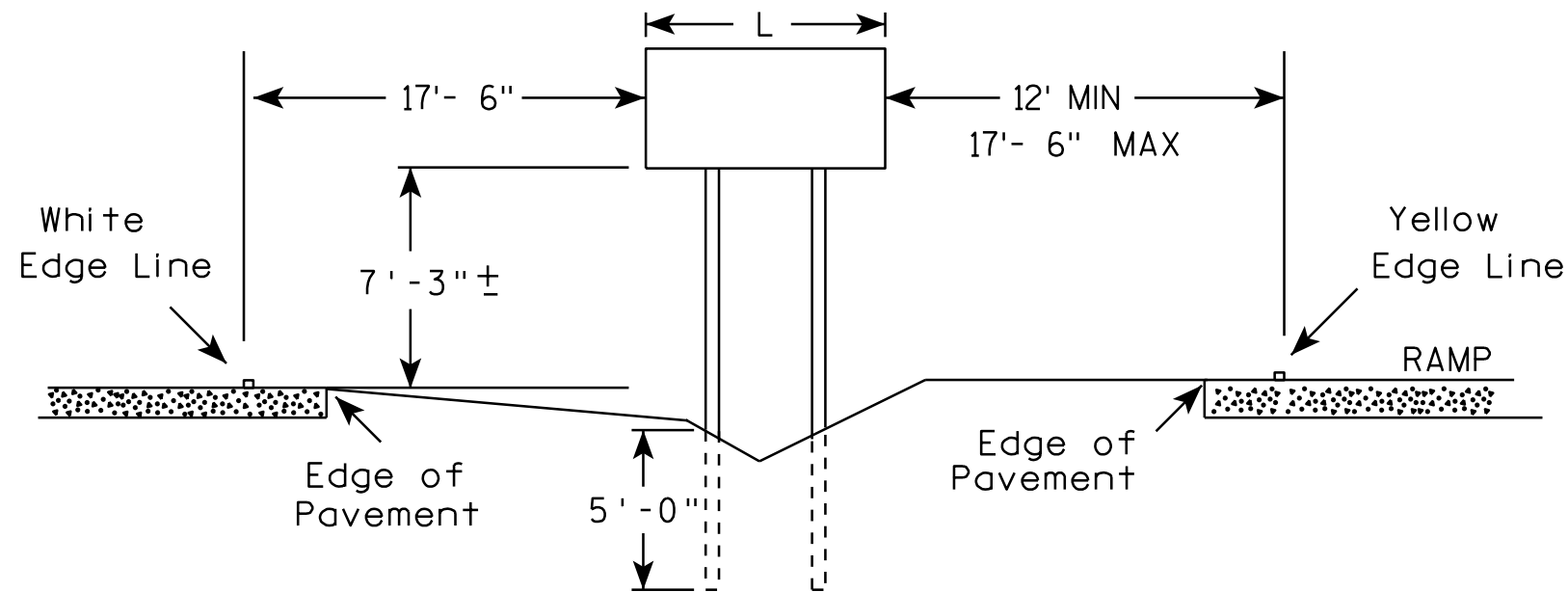
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" x 3"
  - MACHINE BOLTS - 5/16" x 6-1/2" OR 7" LENGTH W/ NUTS

- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" x 3-1/4" LENGTH W/ NUTS
  - RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

- WASHERS (ALL POSTS) -
- 1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL
  - 1-1/4" O.D. x 3/8" I.D. x .080 NYLON FOR ALL TYPE H SIGNS

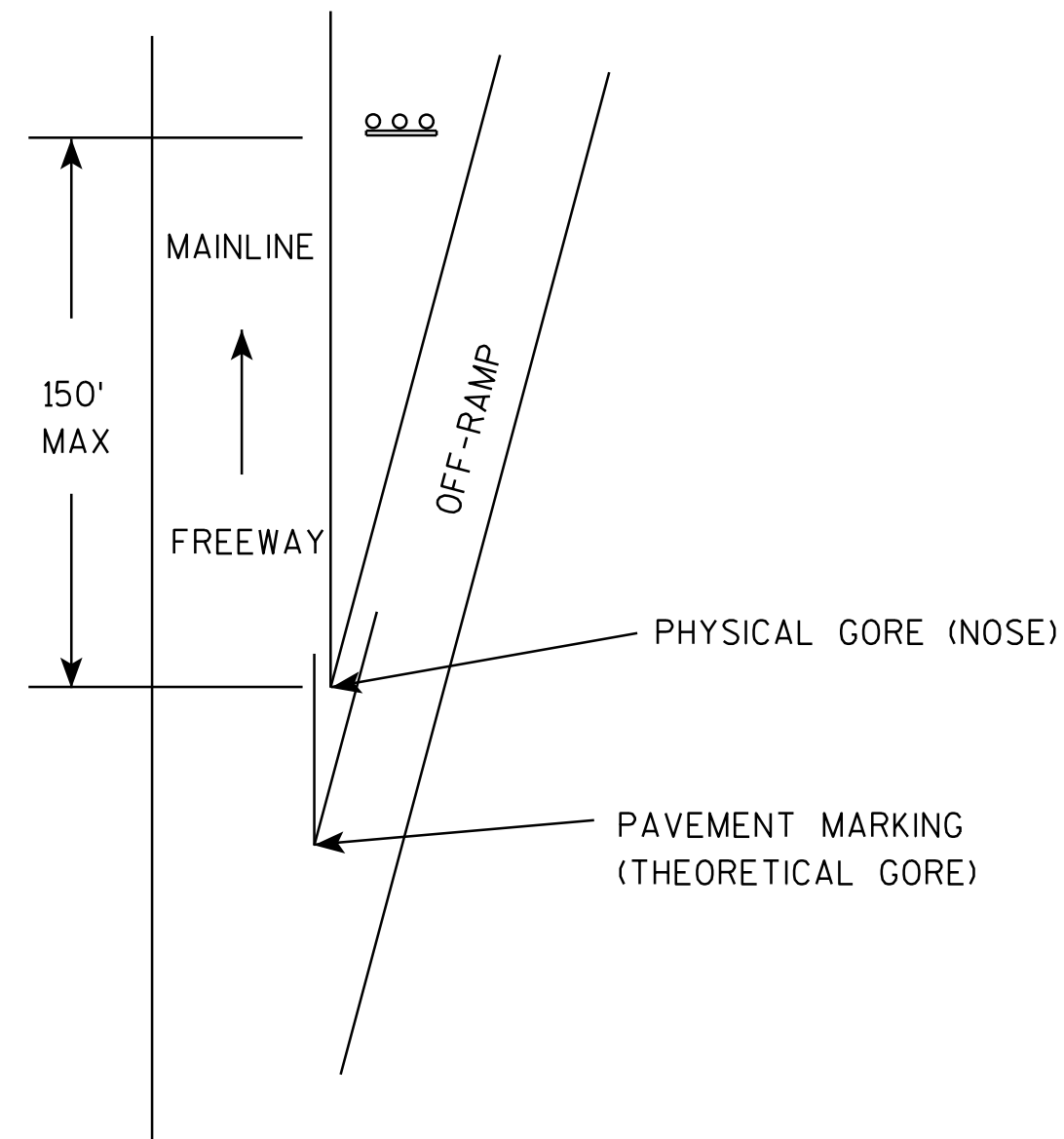
\* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



#### GENERAL NOTES

1. The 150 foot distance from the physical gore (where pavement ends) will normally provide the offsets as shown.
2. If roadway geometrics permit, the sign may be closer than the 150 foot distance as long as the offsets are maintained.
3. At no time shall the location be greater than 150 feet. If the normal offsets cannot be maintained, they can be reduced to 6 feet from the edge of the paved shoulder (both freeway and ramp).
4. The offset from edge of sign to the yellow edge line on the ramp is shown as a minimum of 12 feet and a maximum of 17 feet, 6 inches. Preference is adhering to the maximum rather than the minimum dimension.
5. When L is equal to or exceeds 10 feet, use 3 posts as per A4-4.
6. The ( $\pm$ ) tolerance for the mounting height is 3 inches.



TYPICAL INSTALLATION  
OF TYPE II SIGNS ON  
WOOD POSTS IN GORE

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 2/06/14 PLATE NO. A4-2.3

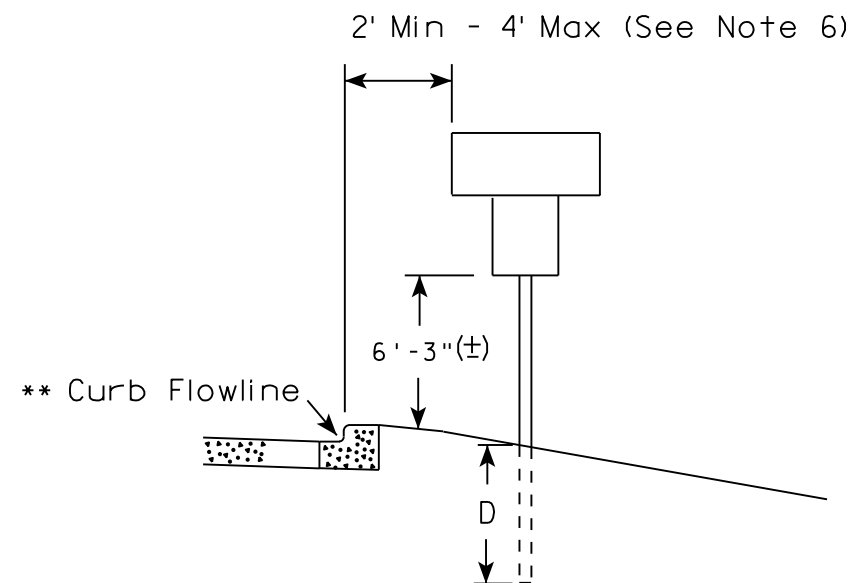
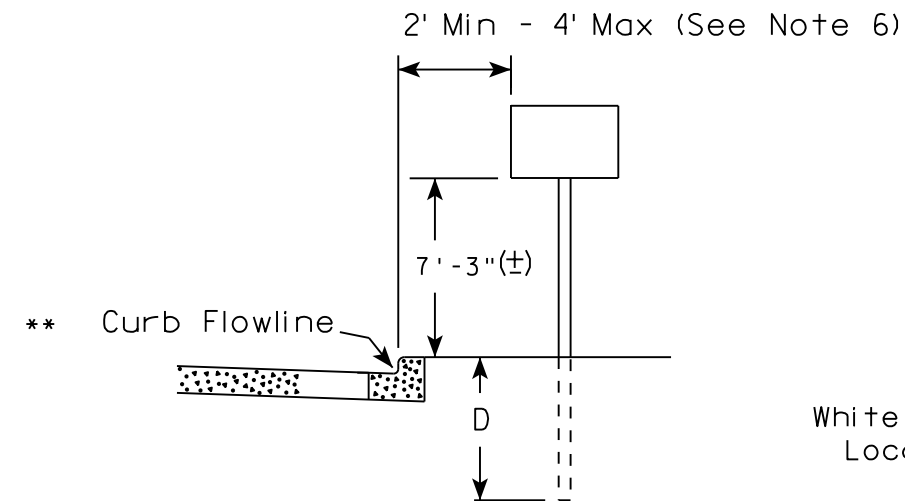
PROJECT NO:

SHEET NO:

E

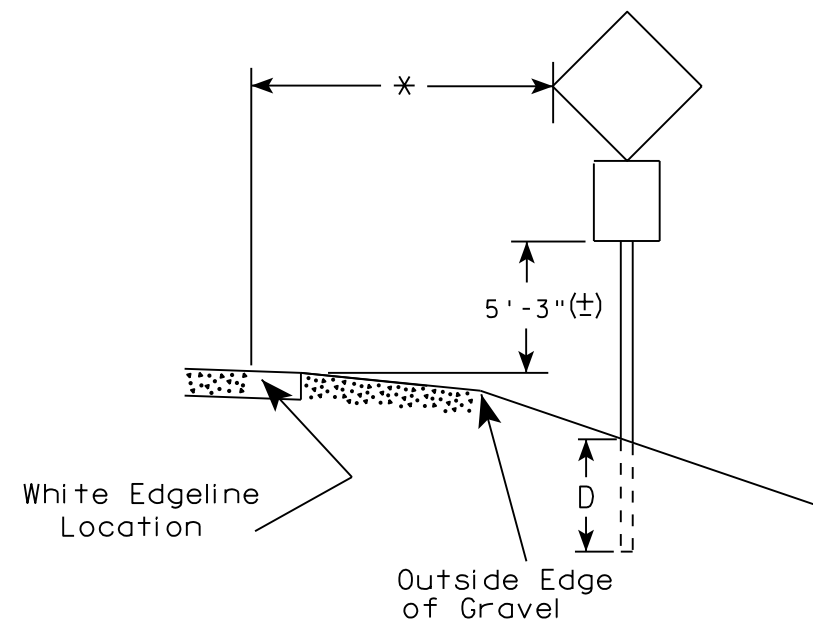
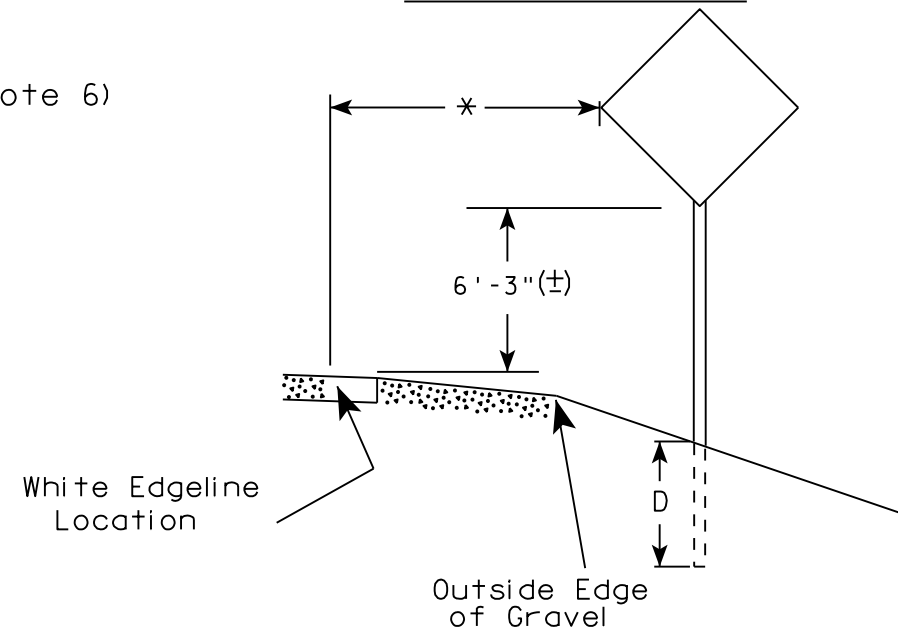


## URBAN AREA



✱✱ The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

## RURAL AREA (See Note 2)



\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

## GENERAL NOTES

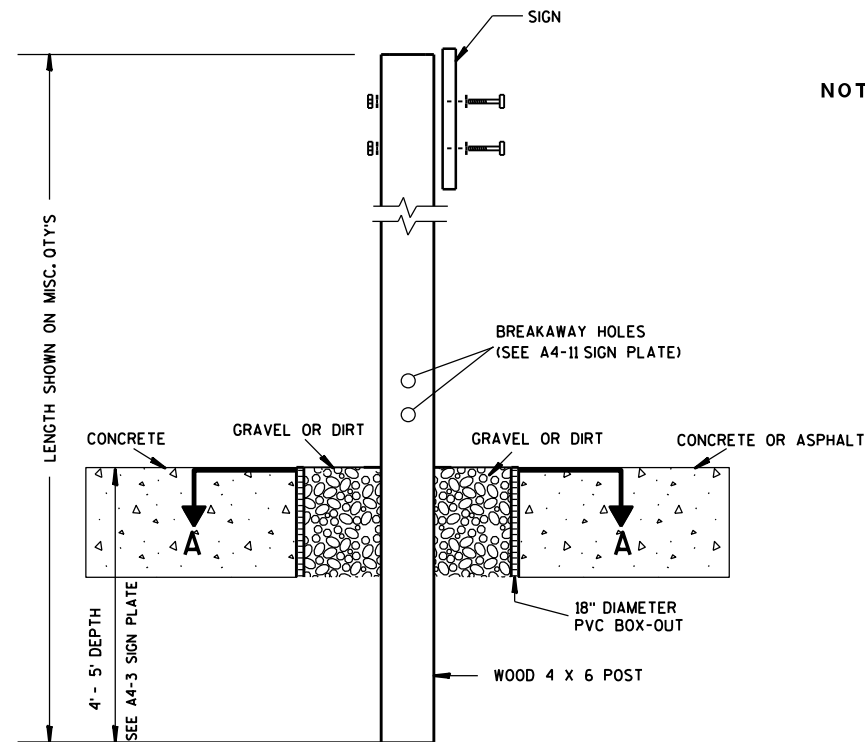
1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. J-Assemblies are considered to be one sign for mounting height.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

TYPICAL INSTALLATION  
OF PERMANENT TYPE II  
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

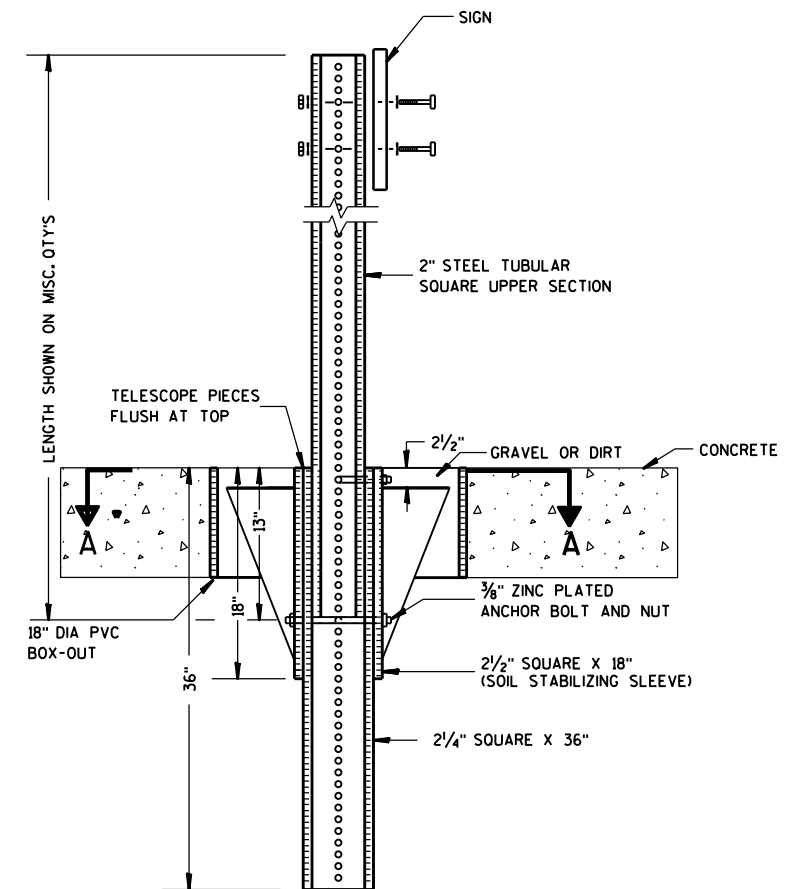
DATE 8/21/17 PLATE NO. A4-3.21



### ELEVATION VIEW

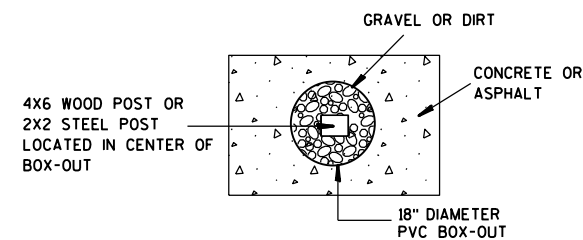
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



### ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



### PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST  
BOX-OUTS  
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

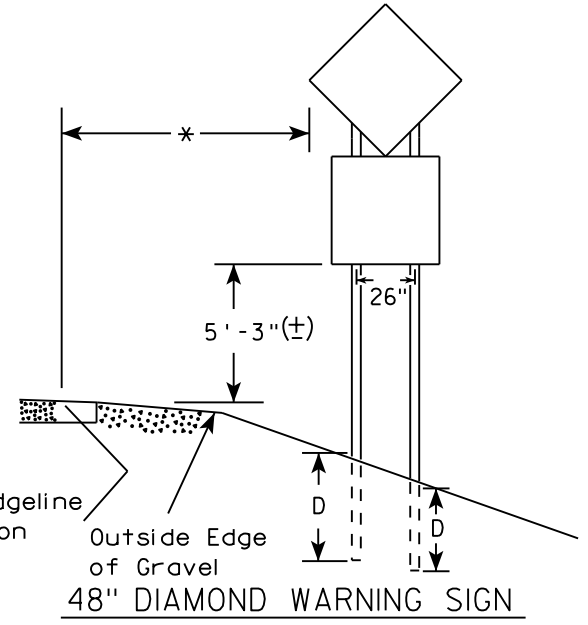
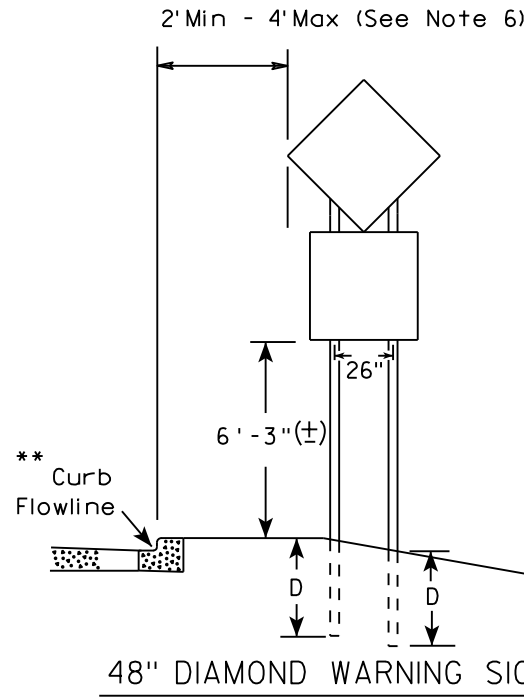
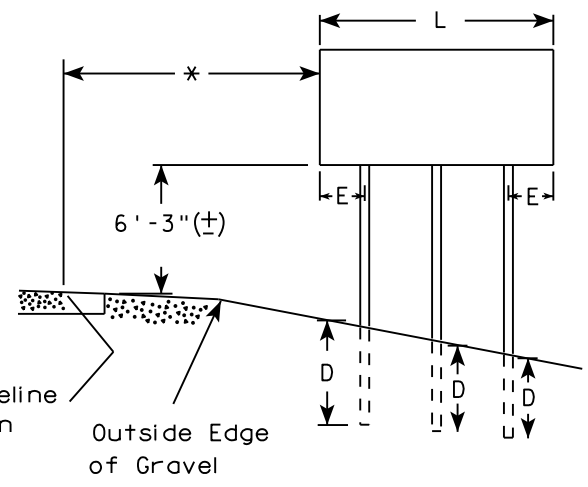
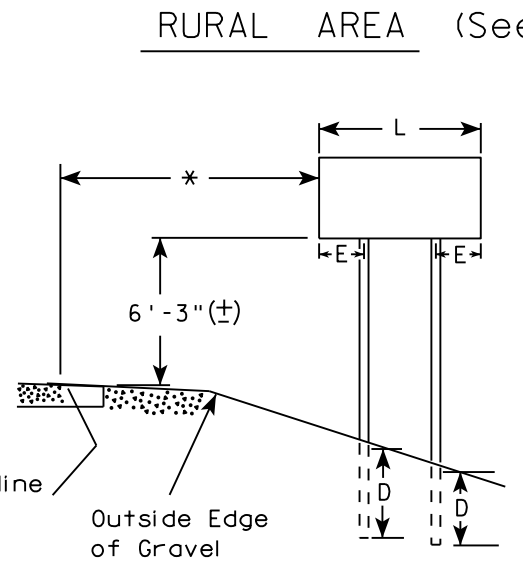
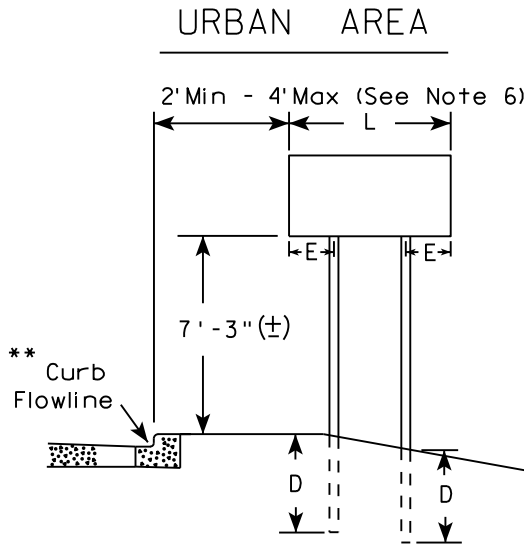
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



- GENERAL NOTES
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
  2. See tables below for required number of posts.
  3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
  4. The (±) tolerance for mounting height is 3 inches.
  5. J-Assemblies are considered to be one sign for mounting height.
  6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
  7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
  8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

\*\*\* See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

\*\*\*

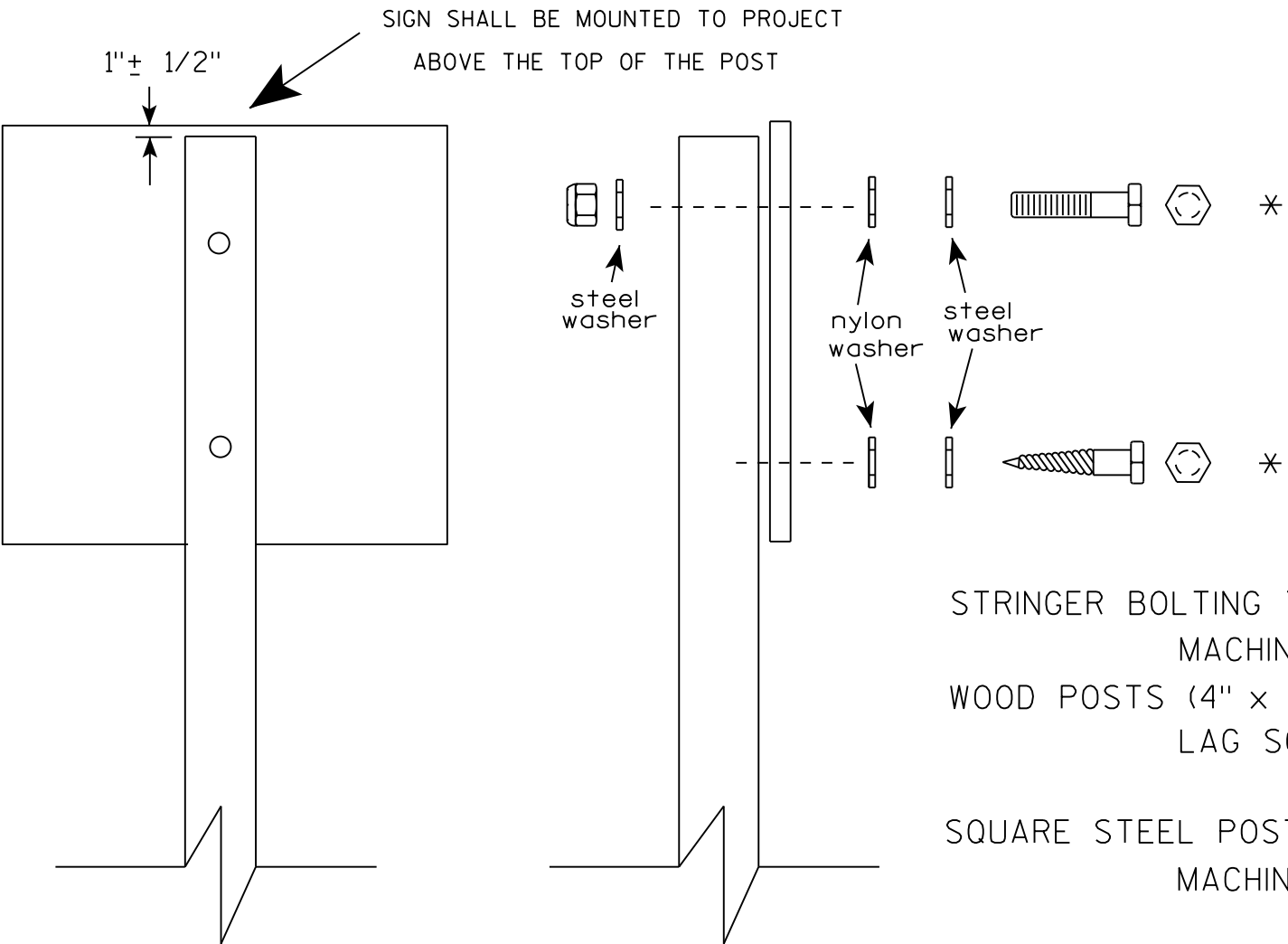
SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 8/21/17	PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

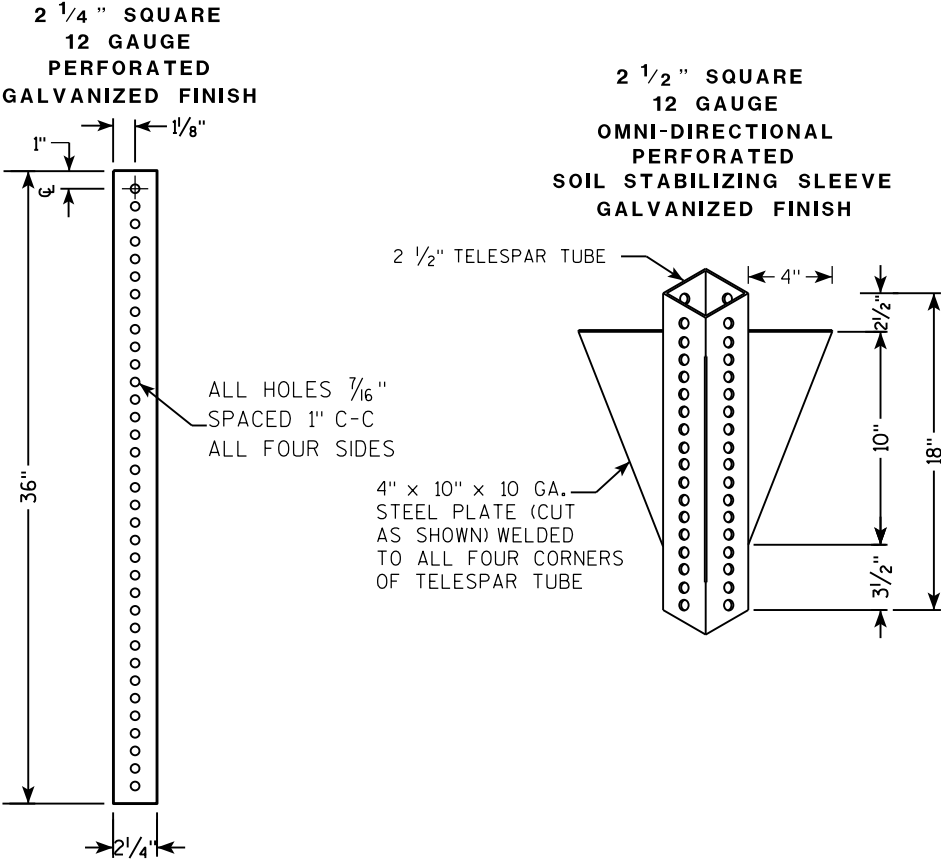
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
  - 3/8" X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
  - 3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
  - 1-1/4" O.D. X 3/8" I.D. X .080 NYLON

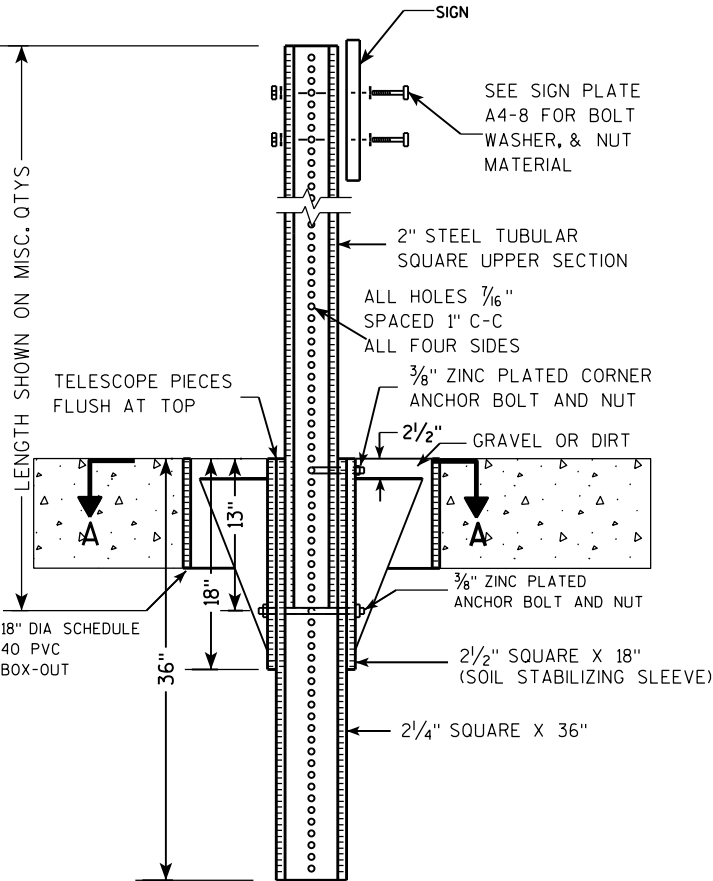
\* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 8/11/16	PLATE NO. A4-8.8

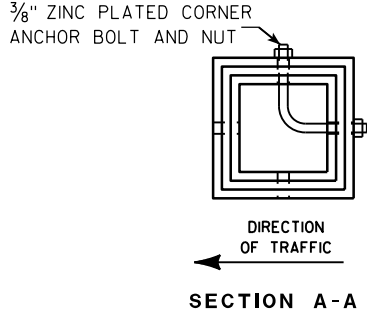
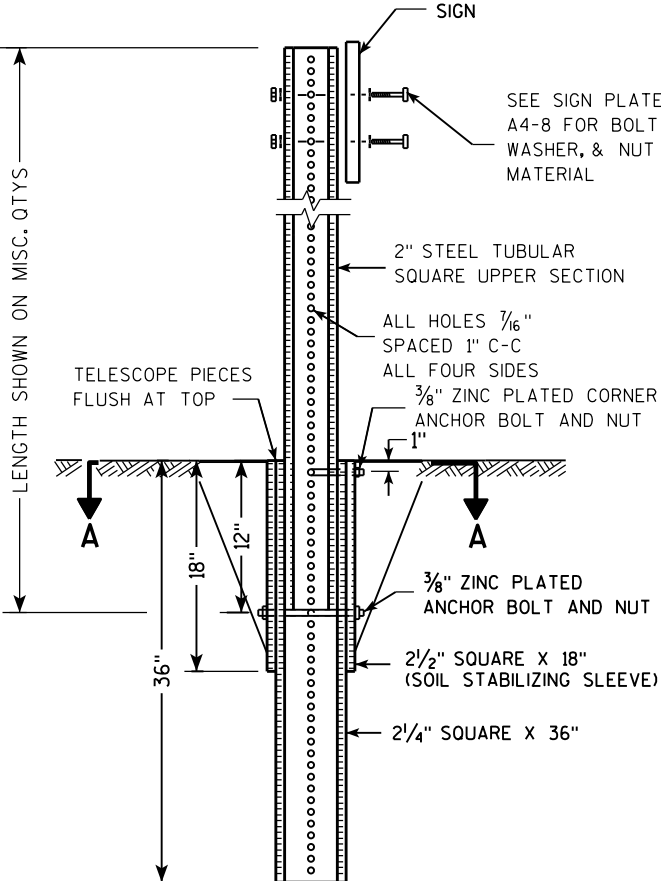
TELESCOPIC TUBING ANCHORS  
TWO PIECE SYSTEM



DETAIL OF TUBULAR STEEL SIGN POST  
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST  
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

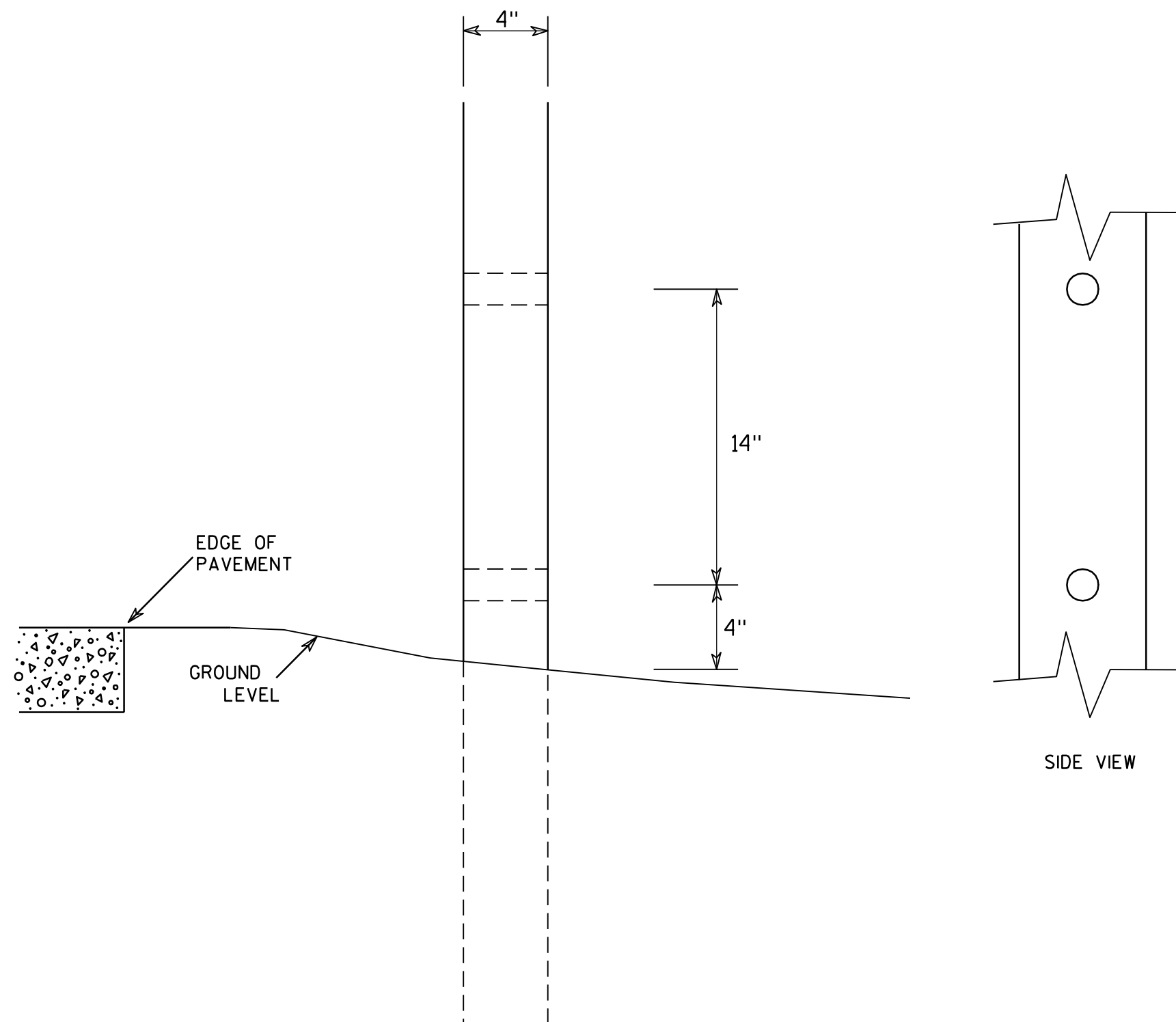
TUBULAR STEEL  
SIGN POST  
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

7



## GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

SIDE VIEW

4 X 6 WOOD POST  
MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Chester J Spang  
for State Traffic Engineer

DATE 3/27/97 PLATE NO. A4-11.2

DATE 3/27/97 PLATE NO. A4-11.2

PROJECT NO:

HWY:

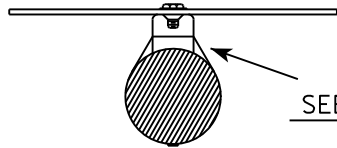
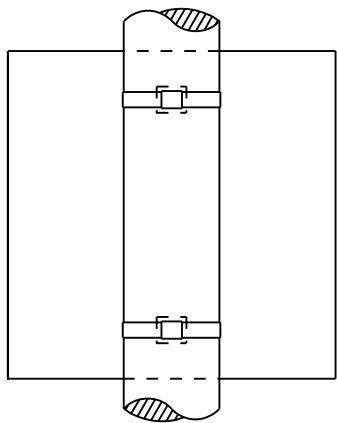
COUNTY:

SHEET NO:
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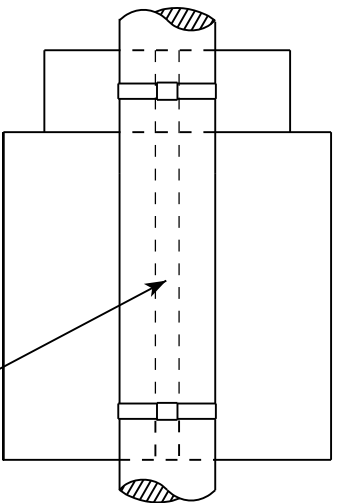
BANDING

SINGLE SIGN

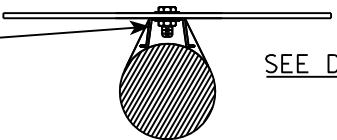


SEE DETAIL A

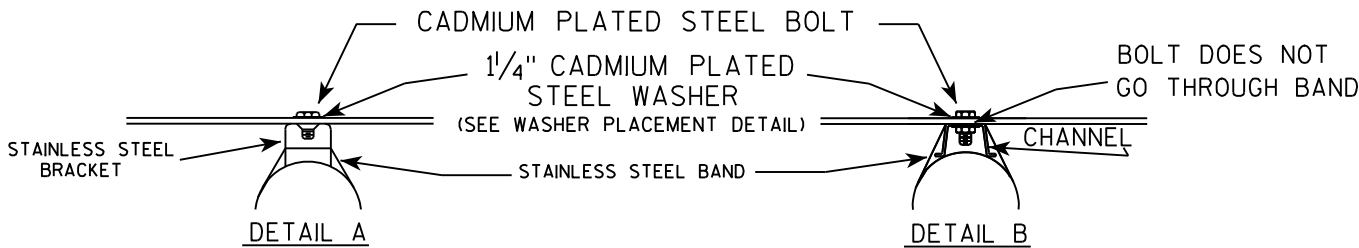
"J" ASSEMBLY



CHANNEL  
SEE TYPICAL PANEL  
INSTALLATION SHEET



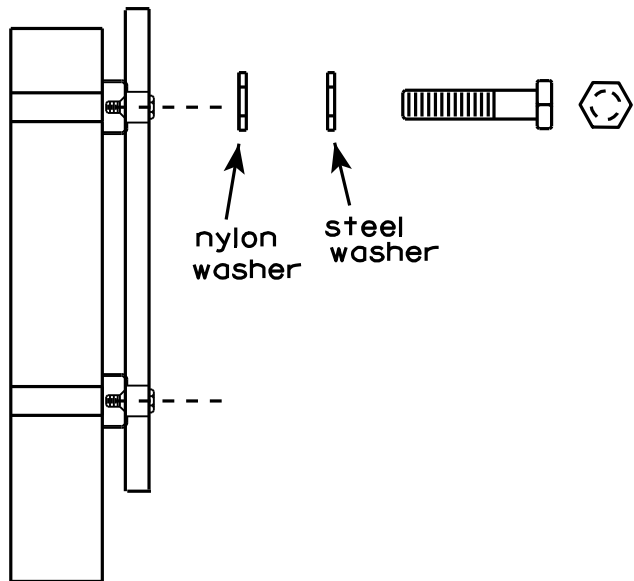
SEE DETAIL B



GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be  $\frac{3}{4}$ " in width and 0.025" thickness.

WASHER PLACEMENT



nylon washer

steel washer

WASHERS (ALL POSTS) -  
1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ " STEEL  
1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X .080 NYLON  
FOR ALL TYPE H SIGNS

STANDARD SIGN  
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Matthew R. Rauch*  
for State Traffic Engineer

DATE 8/16/13

PLATE NO. A5-9.3

PROJECT NO:

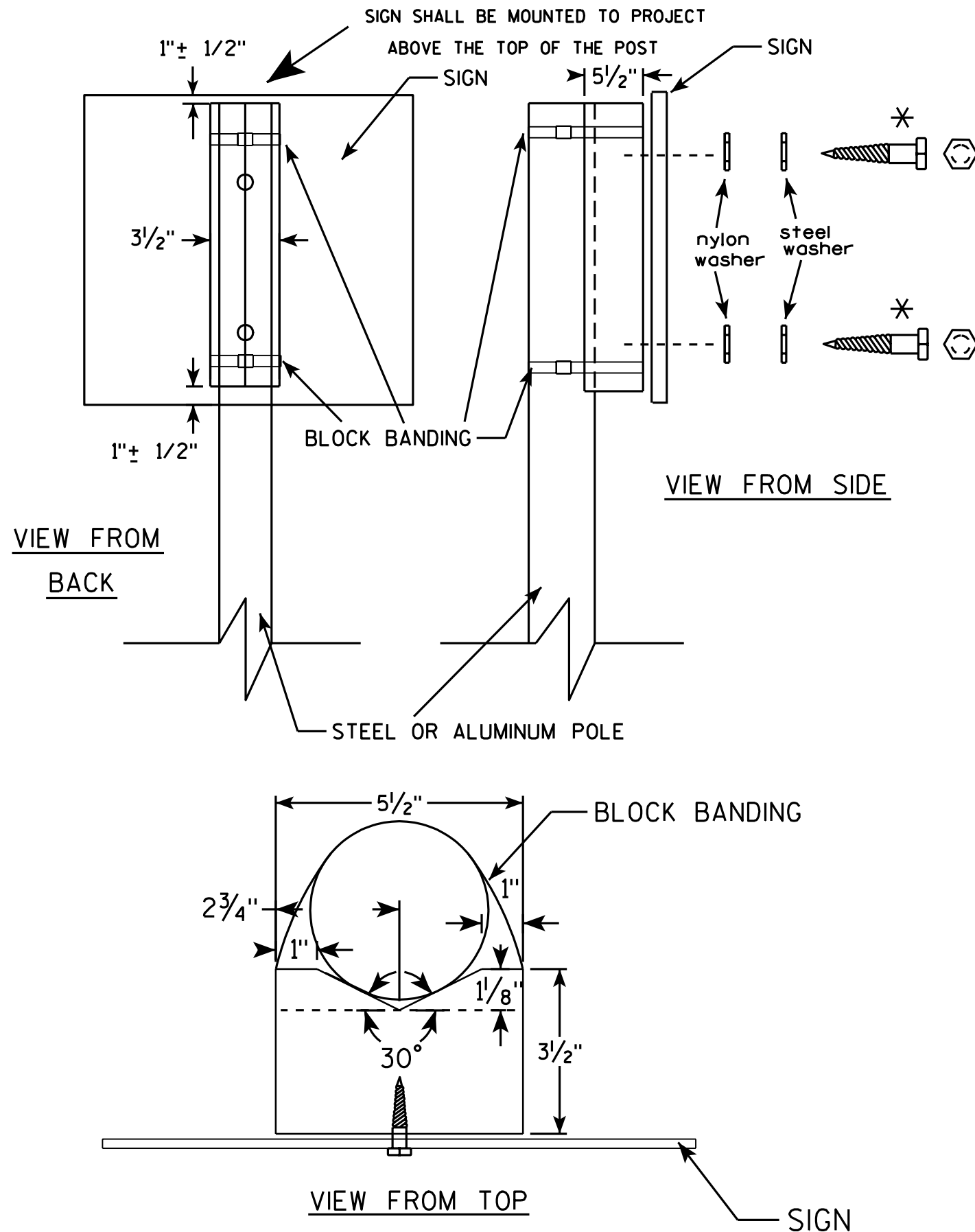
HWY:

COUNTY:

SHEET NO:

E





## GENERAL NOTES

1. WOOD 4"x6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D, or
  - b. Cadmium plated in accordance with ASTM Designation : B 766 TYPE 3, Class 12, or
  - c. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"

BLOCK BANDING DETAIL  
( V-BLOCK OPTION )

WISCONSIN DEPT OF TRANSPORTATION

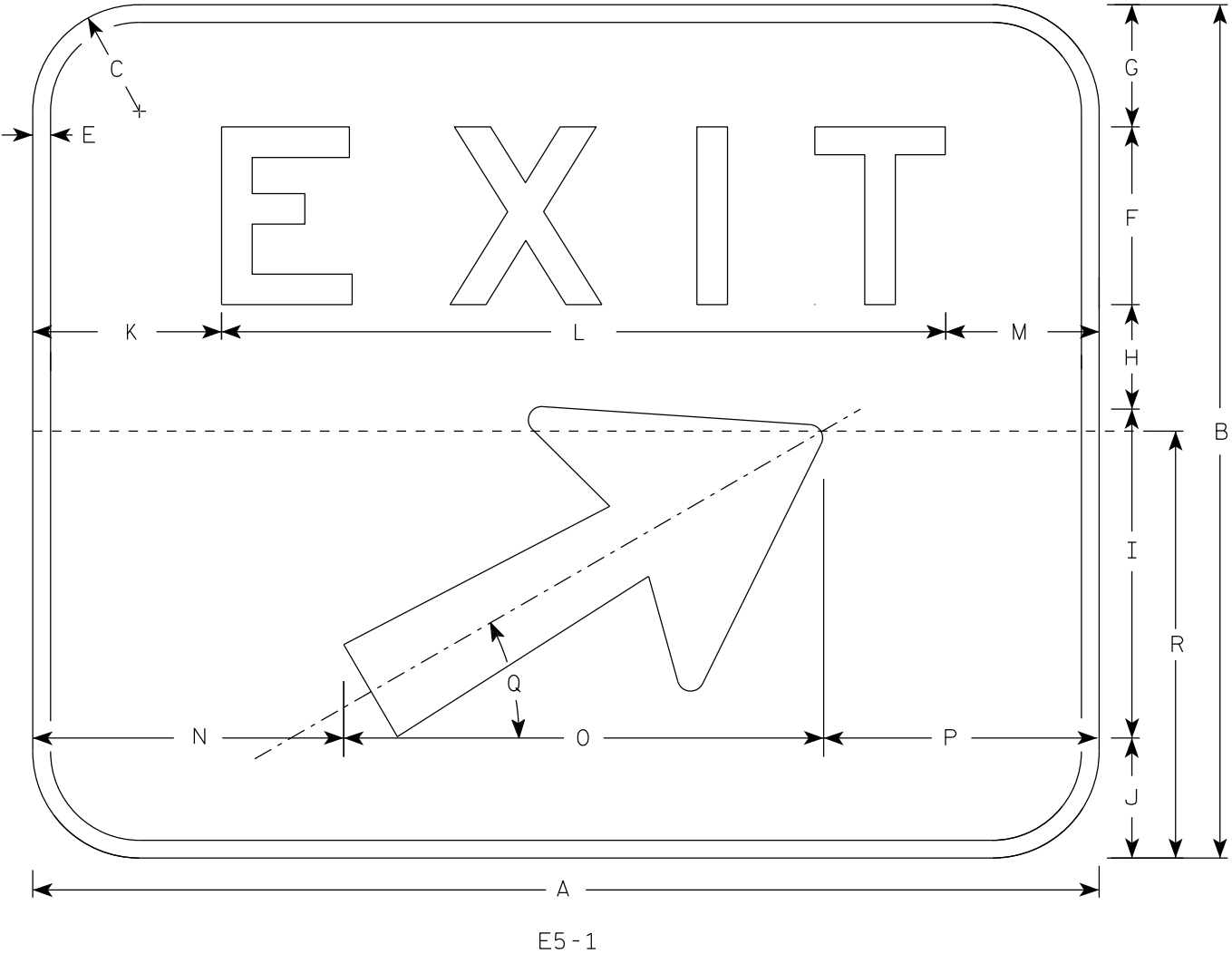
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 7/12/07 PLATE NO. A5-10.1

PROJECT NO:

SHEET NO:

E



NOTES

- 1. Sign is Type II - Type H reflective
- 2. Color:  
Background - Green  
Message - White
- 3. Message Series - E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. When Base material for this sign is plywood sign shall be split into two separate pieces for the 72 x 60 size as shown on the detail by the dashed line (-----).
- 6. Arrow is Type "A" from sign plate A1-1.
- 7. As per the Standard Spec's, this sign shall not have a vertical joint.
- 8. Size 3 E5-1 shall only be used in a Work Zone application with a Temporary Sign Support

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2																											
3	48	48	6		1	10	6 7/8	5 7/8	18 1/2	6 3/4	8 1/2	31 1/8	8 3/8	11 1/2	27	9 1/2	30°	30									16.0
4	60	48	6		1	10	6 7/8	5 7/8	18 1/2	6 3/4	10 5/8	40 3/4	8 5/8	17 1/2	27	15 1/2	30°	30									20.0
5	72	60	6		1	12	9 3/4	10	18 1/2	9 3/4	13 1/4	48 1/2	10 1/4	23 1/2	27	21 1/2	30°	30									30.0

PROJECT NO:

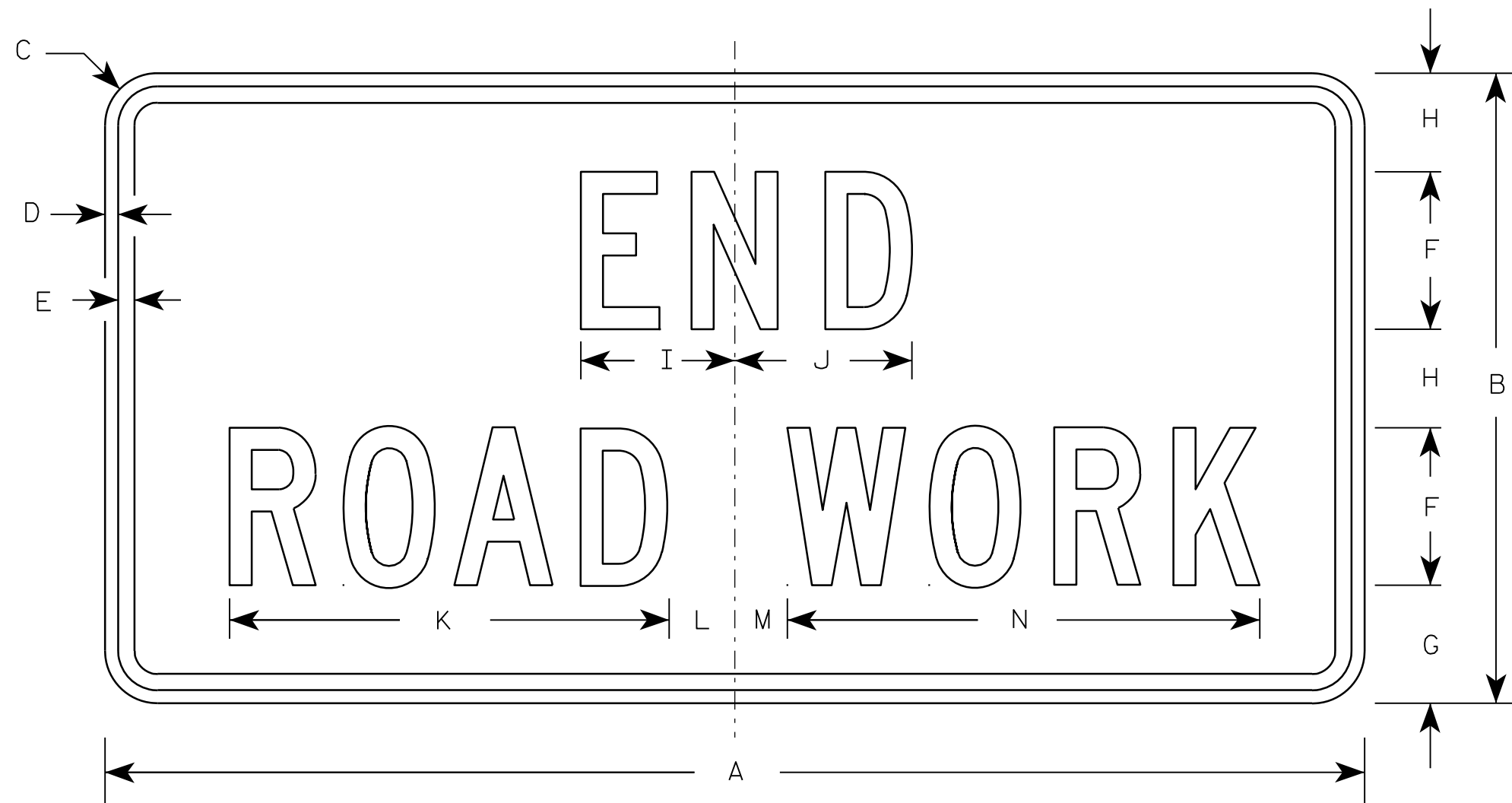
HWY:

COUNTY:

SHEET NO:

E

7



G20-2A

Metric equivalent  
for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m <sup>2</sup>
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

NOTES

- Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:  
Background - Orange  
Message - Black
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

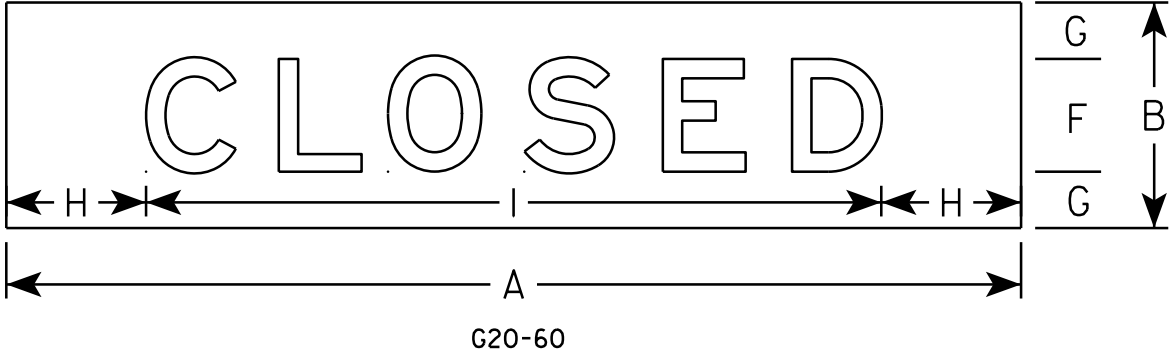
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

- 1. Sign is Type II- Type F Reflective
- 2. Color:  
Background - Orange  
Message - Black
- 3. Message Series - E
- 4. Material shall be .040 aluminum

7

7

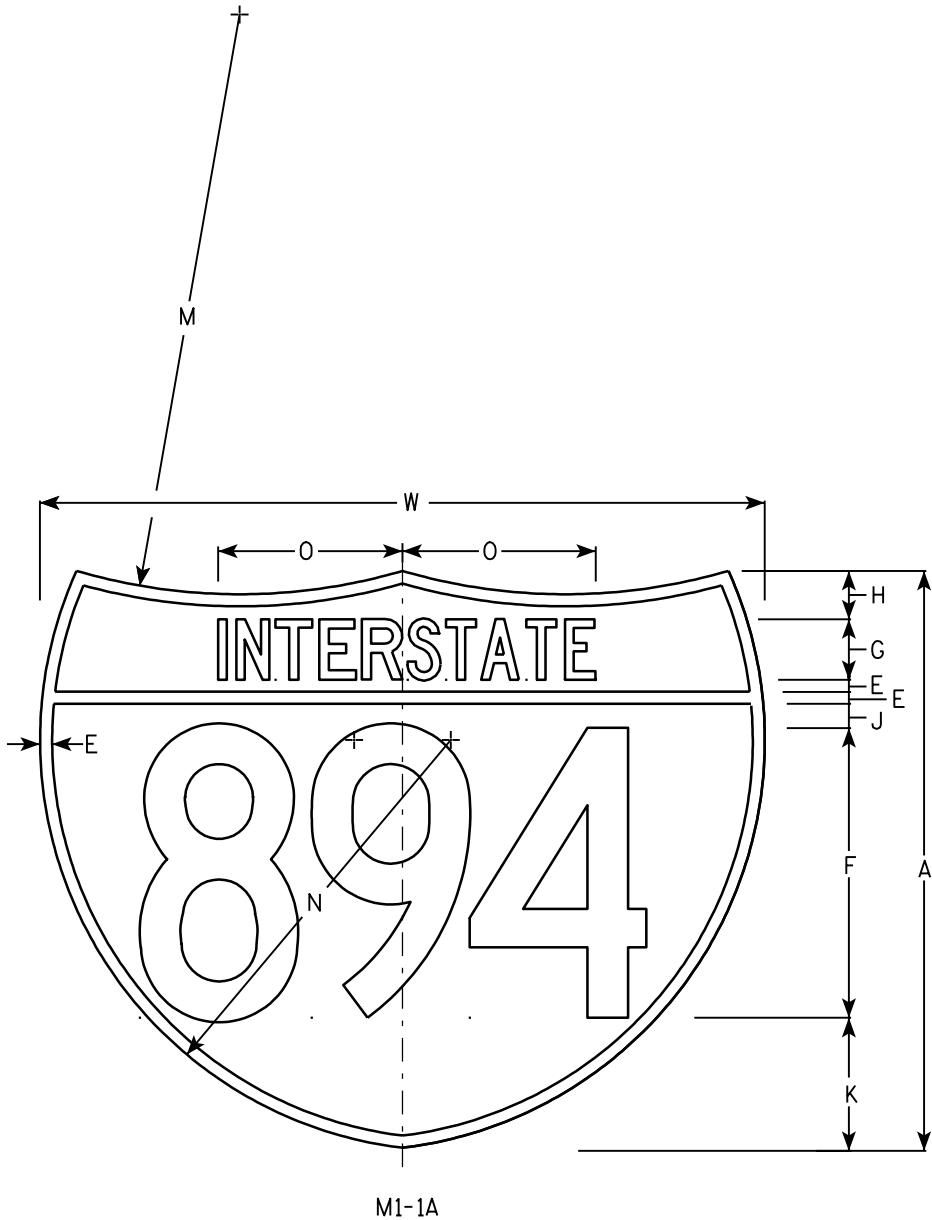
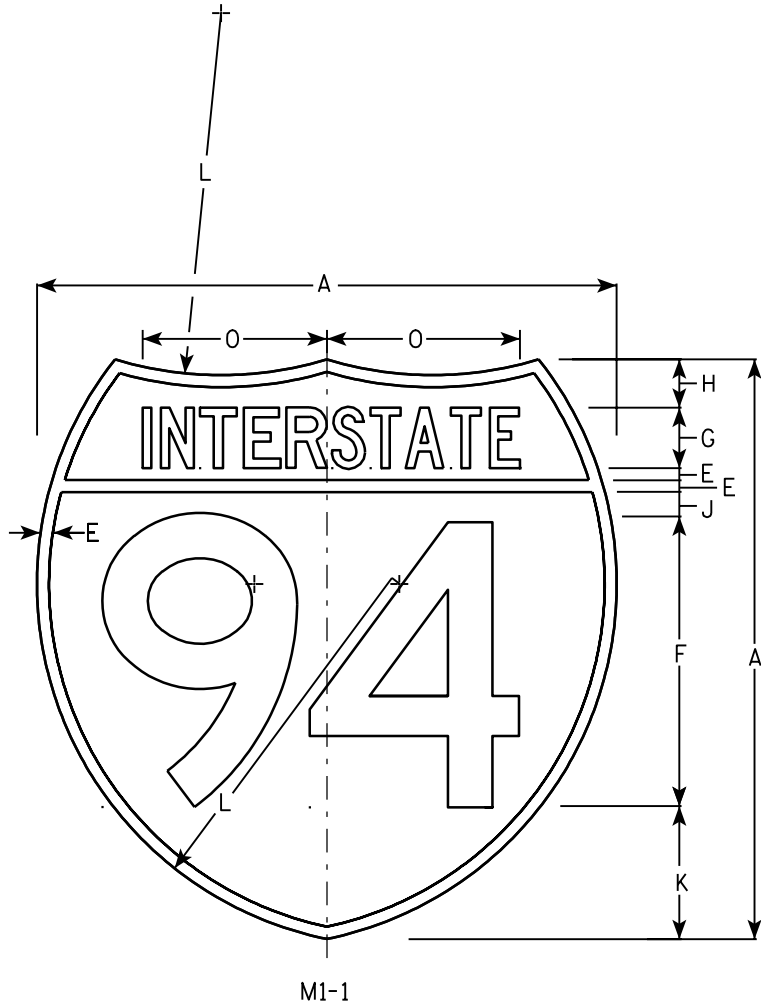
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2																											
3																											
4	108	24				12	6	14 7/8	78 1/4																		18.0
5																											

STANDARD SIGN  
G20-60

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/13/15 PLATE NO. G20-60.1



NOTES

- 1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:  
Background - Top Red - Bottom Blue (See Note 6)  
Message - White - See Note 6
- 3. Message Series - See note 5
- 4. Substitute appropriate numerals & adjust spacing as per plate A10-1.
- 5. M1-1 - Numerals - D  
Interstate - C  
M1-1A - All copy - C
- 6. Permanent Signs  
Message - Type H Reflective  
Detour or other temporary signs  
Background - Reflective  
Message - Reflective

Metric equivalent for these signs are:

SIZE	M1 - 1	SIZE	M1 - 1A
1			
2	600 mm X 600 mm	2	600 mm X 750 mm
3	900 mm X 900 mm	3	900 mm X 1125 mm
4	900 mm X 900 mm	4	900 mm X 1125 mm
5	900 mm X 900 mm	5	900 mm X 1125 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Area sq. ft.	Area sq. ft.	Area m <sup>2</sup>	Area m <sup>2</sup>
1																													
2	24				1/2	12	2 1/2	2		1	5 1/2	15	24	17	7 7/8								30			3.13	3.91	.36	.46
3	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 3/4								45			7.03	8.79	.81	1.05
4	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 3/4								45			7.03	8.79	.81	1.05
5	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 3/4								45			7.03	8.79	.81	1.05

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

INTERSTATE ROUTE MARKER  
M1-1 FOR ASSEMBLIES

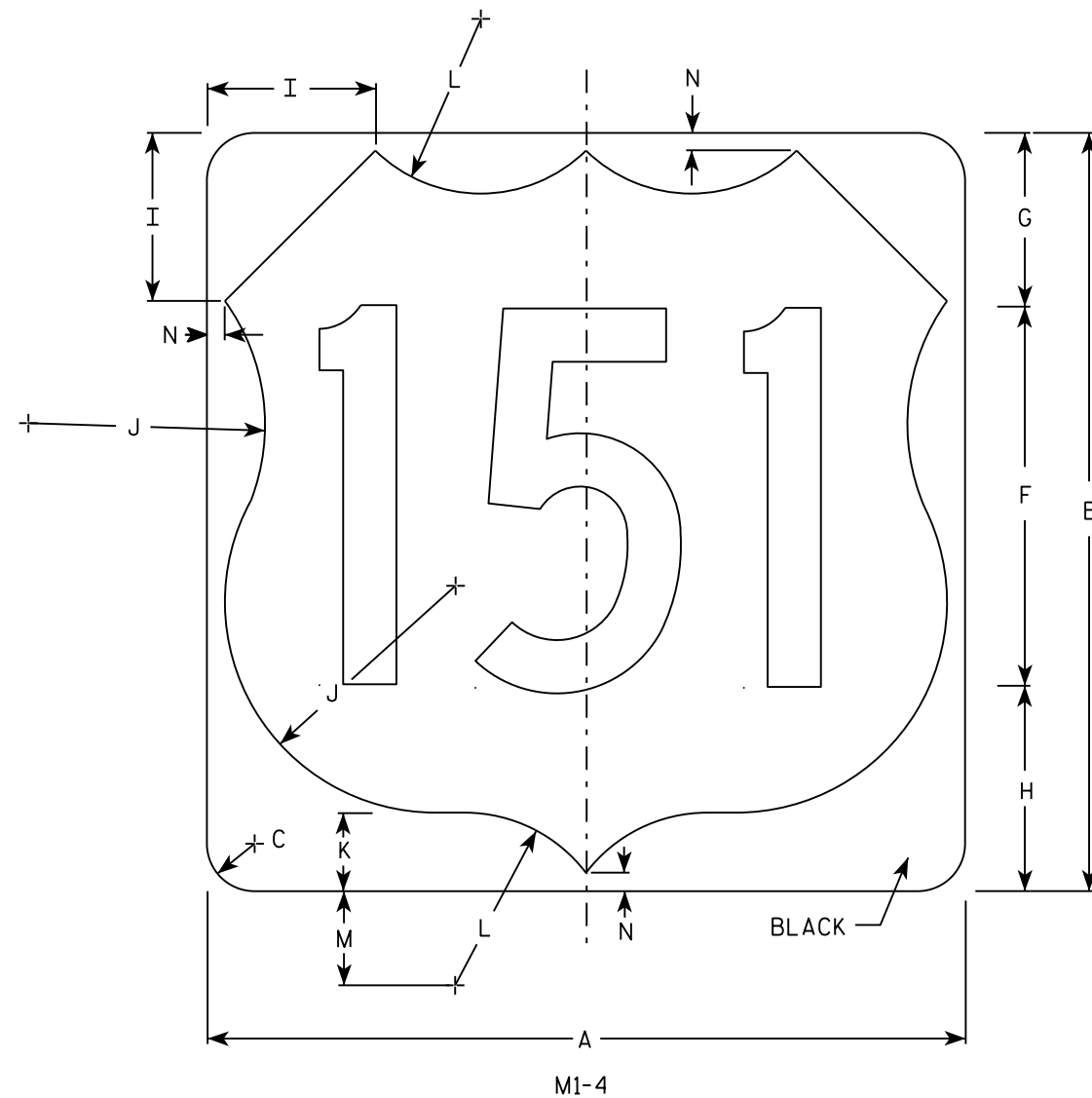
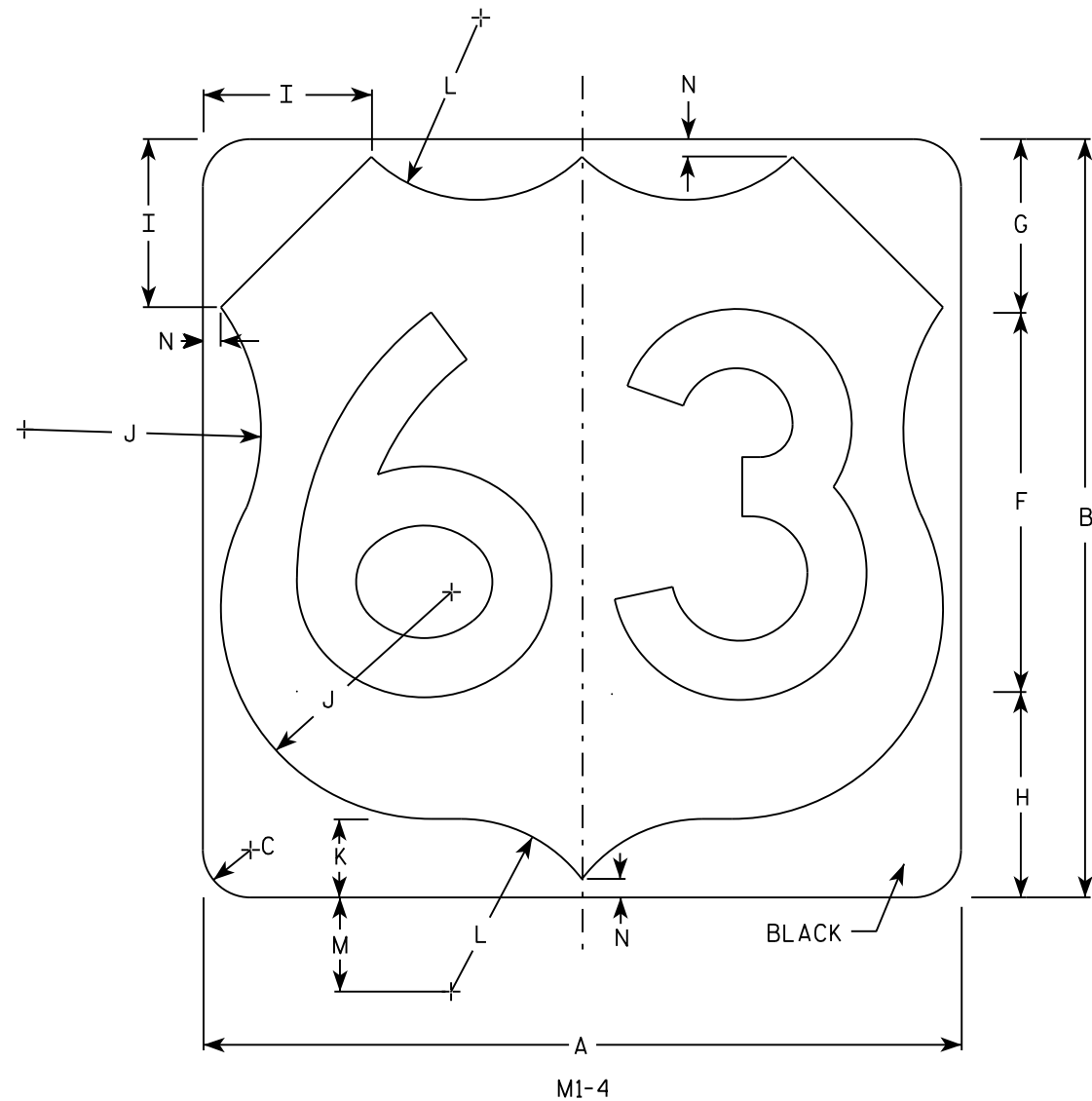
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 08/23/05 PLATE NO. M1-1.8

NOTES

1. Sign is Type II - Type H Reflective
2. Color:  
Background - White  
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Areq. sq. ft.
1																											
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0

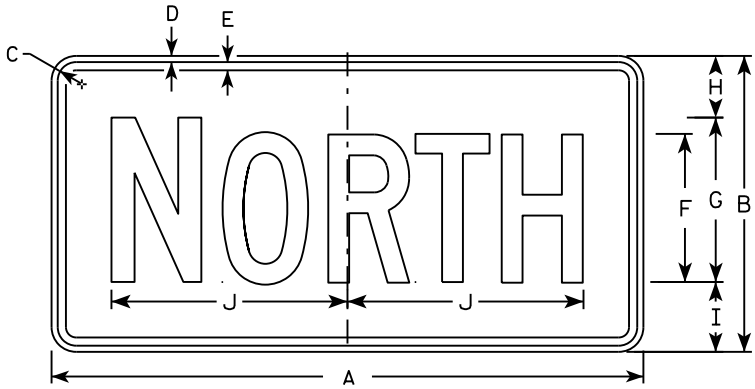
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E
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USH MARKER  
M1-4 FOR ASSEMBLIES

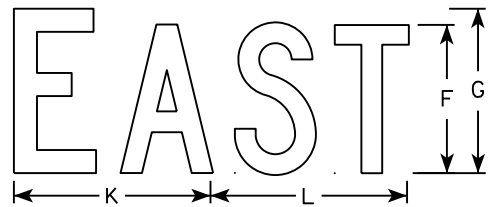
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

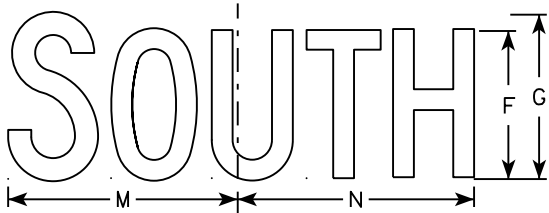
DATE 3/16/18 PLATE NO. M1-4.10



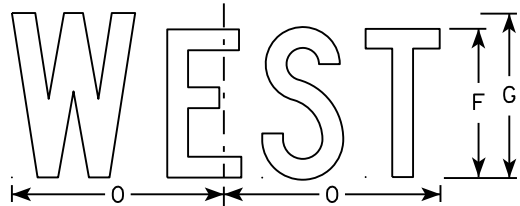
M3-1  
MM3-1  
MP3-1



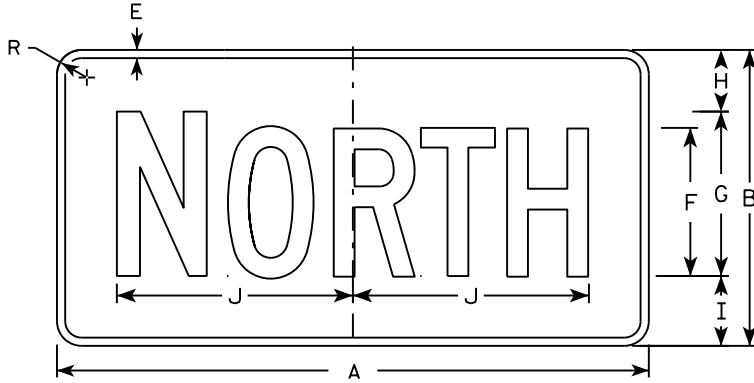
M3-2  
MM3-2  
MP3-2



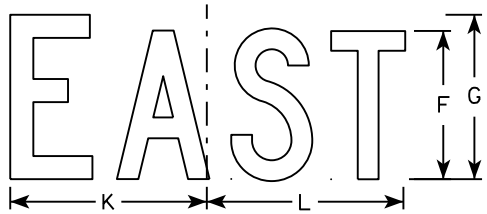
M3-3  
MM3-3  
MP3-3



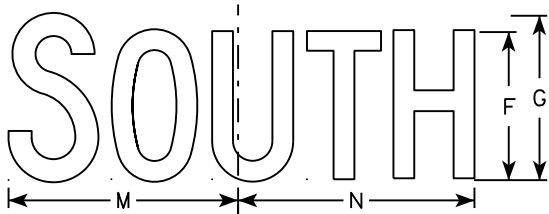
M3-4  
MM3-4  
MP3-4



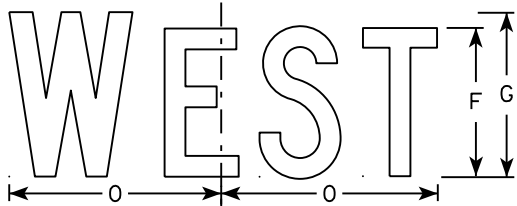
MB3-1  
MK3-1  
MN3-1



MB3-2  
MK3-2  
MN3-2



MB3-3  
MK3-3  
MN3-3



MB3-4  
MK3-4  
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:  
Background - See note 5  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White  
Message - Black  
MB3-1 thru MB3-4 Background - Blue  
Message - White  
MK3-1 thru MK3-4 Background - Green  
Message - White  
MM3-1 thru MM3-4 Background - White  
Message - Green  
MN3-1 thru MN3-4 Background - Brown  
Message - White  
MP3-1 thru MP3-4 Background - White  
Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

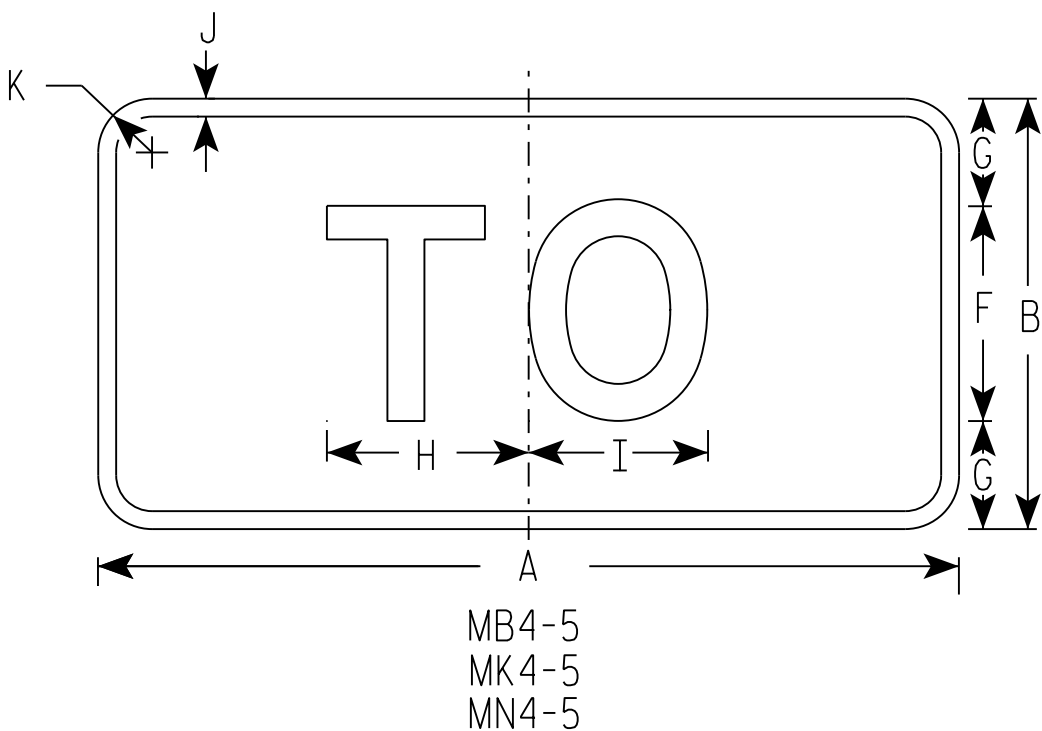
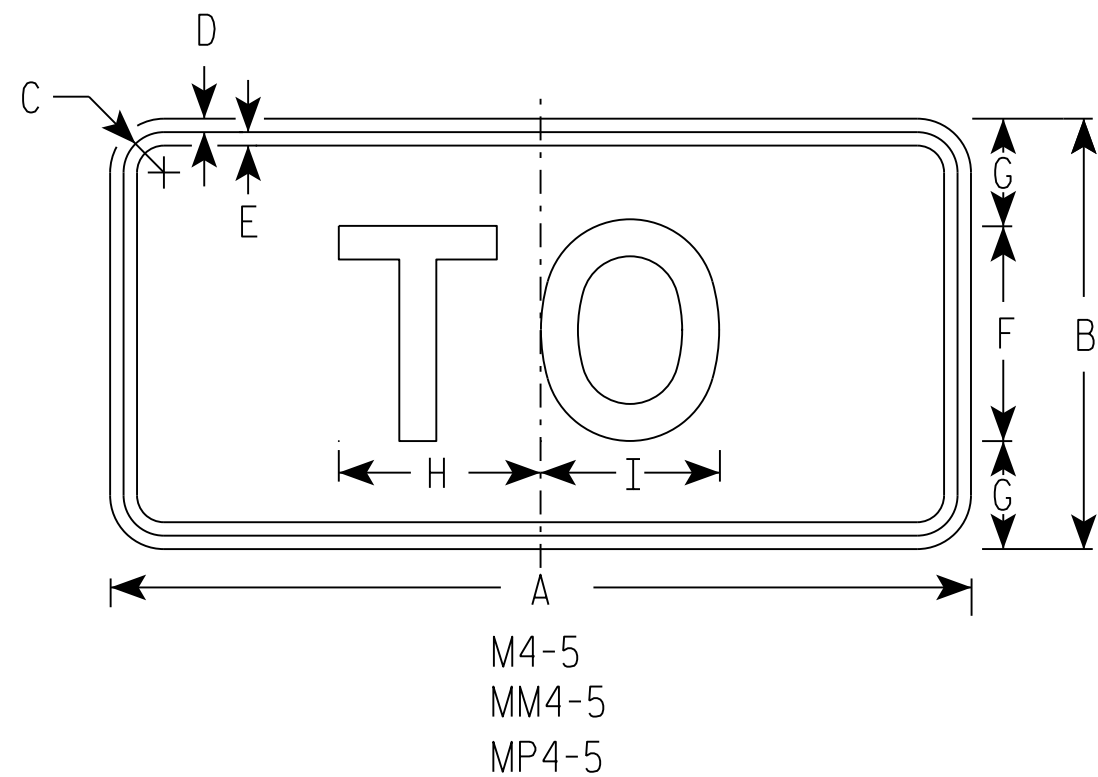
STANDARD SIGNS  
M3-1 thru M3-4  
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14





NOTES

- 1. Sign is Type II - Type H
- 2. Color:
  - Background - See note 5
  - Message - See note 5
- 3. Message Series - E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M4-5 Background - White  
Message - Black
- MB4-5 Background - Blue  
Message - White
- MK4-5 Background - Green  
Message - White
- MM4-5 Background - White  
Message - Green
- MN4-5 Background - Brown  
Message - White
- MP4-5 Background - White  
Message - Blue

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	5 3/8	5 1/4	1/2	1 1/2																2.00
3	36	18	1 3/8	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2	1 1/2																4.5
4	36	18	1 3/8	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2	1 1/2																4.5
5	36	18	1 3/8	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2	1 1/2																4.5

STANDARD SIGN

M4 - 5

WISCONSIN DEPT OF TRANSPORTATION

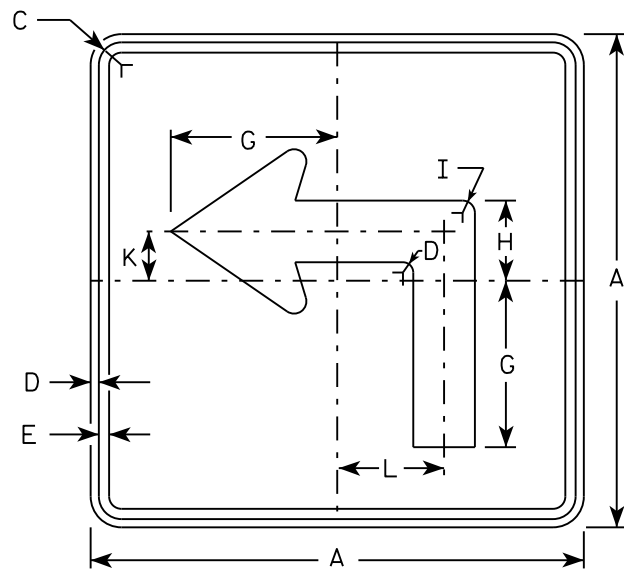
APPROVED

Matthew R. Rauch

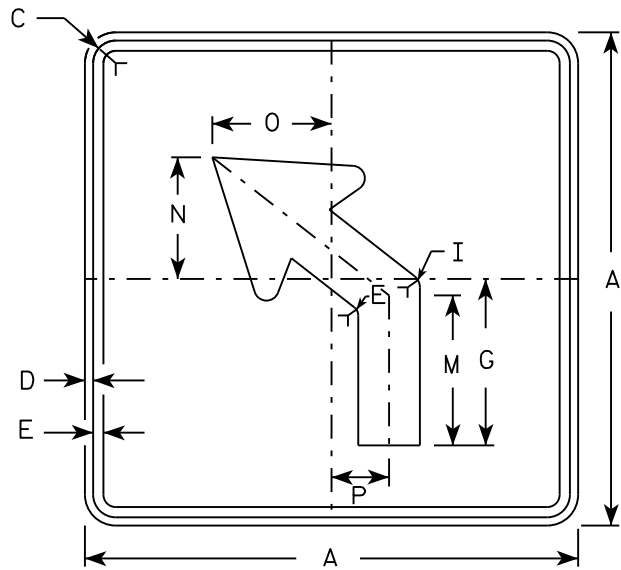
for State Traffic Engineer

DATE 10/15/15

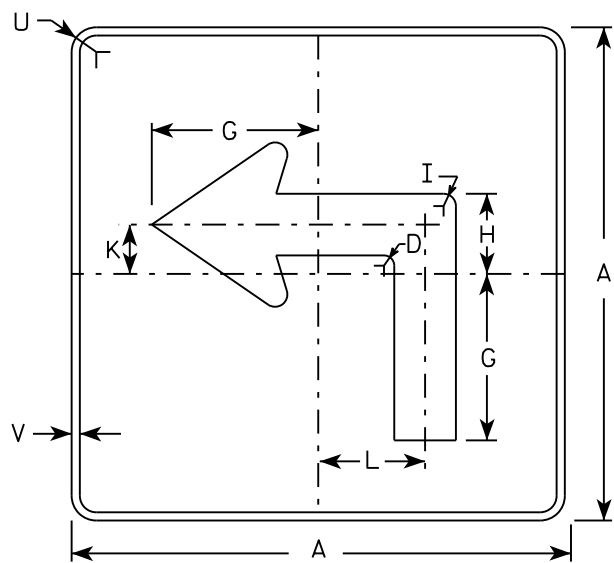
PLATE NO. M4-5.8



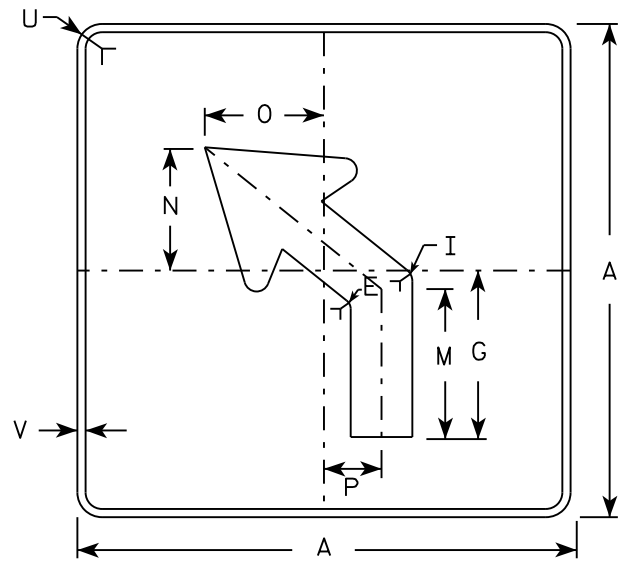
M5-1L  
MM5-1L  
M05-1L  
MP5-1L



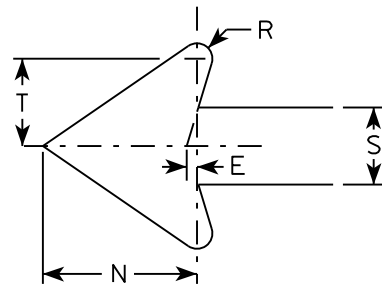
M5-2L  
MM5-2L  
M05-2L  
MP5-2L



MB5-1L  
MK5-1L  
MN5-1L  
MR5-1L



MB5-2L  
MK5-2L  
MN5-2L  
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:  
Background - See note 4  
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M5-1 and M5-2 Background - White  
Message - Black  
MB5-1 and MB5-2 Background - Blue  
Message - White  
MK5-1 and MK5-2 Background - Green  
Message - White  
MM5-1 and MM5-2 Background - White  
Message - Green  
MN5-1 and MN5-2 Background - Brown  
Message - White  
M05-1 and M05-2 Background - Orange - Type F Reflective  
Message - Black  
MP5-1 and MP5-2 Background - White - Type H Reflective  
Message - Blue  
MR5-1 and MR5-2 Background - Brown  
Message - Yellow
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

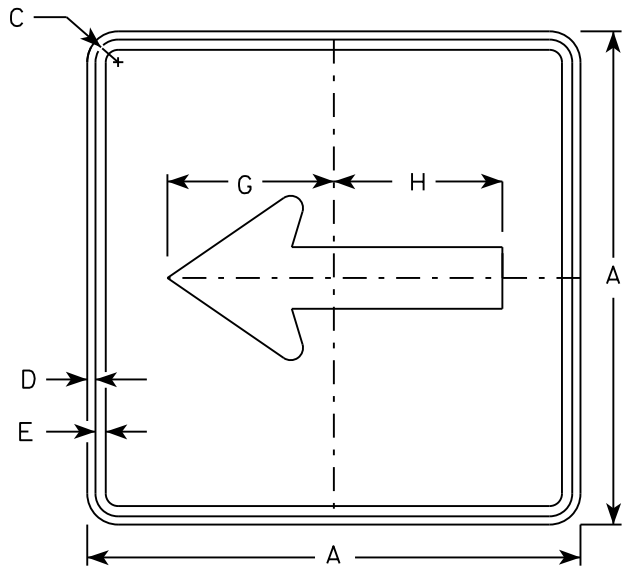
PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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STANDARD SIGN  
M5-1 & M5-2

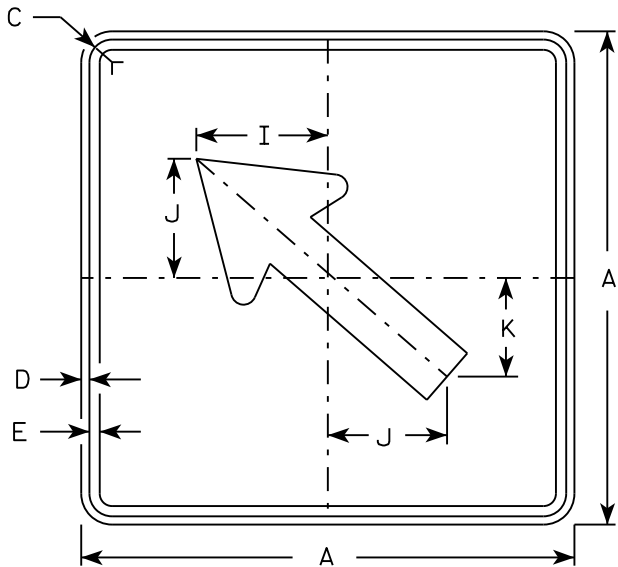
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

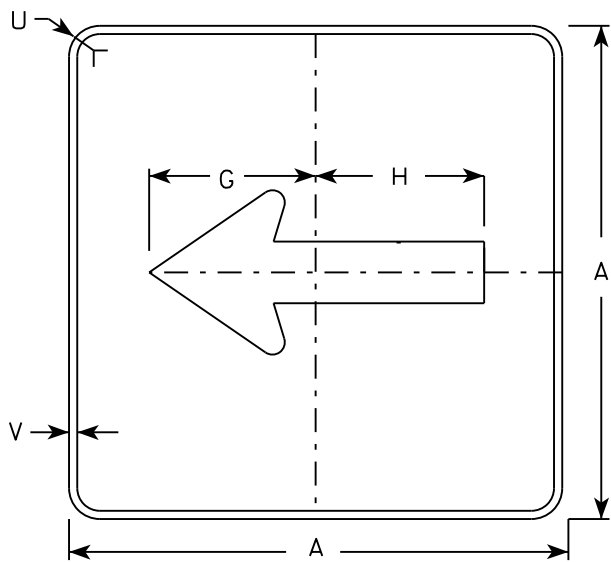
DATE 10/15/15      PLATE NO. M5-1.13



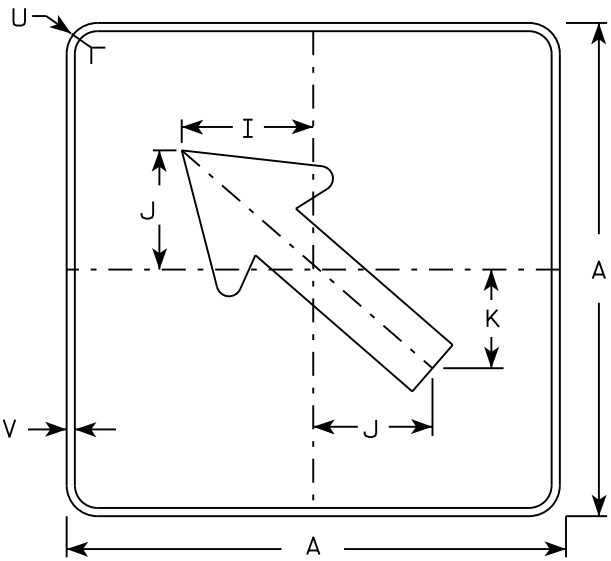
M6 - 1  
MM6 - 1  
M06 - 1  
MP6 - 1



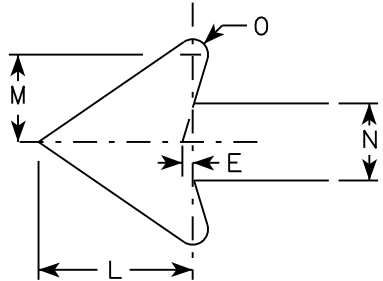
M6 - 2  
MM6 - 2  
M06 - 2  
MP6 - 2



MB6 - 1  
MK6 - 1  
MN6 - 1  
MR6 - 1



MB6 - 2  
MK6 - 2  
MN6 - 2  
MR6 - 2



NOTES

- Signs are Type II - Type H except as Shown
- Color:  
Background - See note 4  
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White  
Message - Black  
MB6-1 and MB6-2 Background - Blue  
Message - White  
MK6-1 and MK6-2 Background - Green  
Message - White  
MM6-1 and MM6-2 Background - White  
Message - Green  
MN6-1 and MN6-2 Background - Brown  
Message - White  
M06-1 and M06-2 Background - Orange - Type F Reflective  
Message - Black  
MP6-1 and MP6-2 Background - White  
Message - Blue  
MR6-1 and MR6-2 Background - Brown  
Message - Yellow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

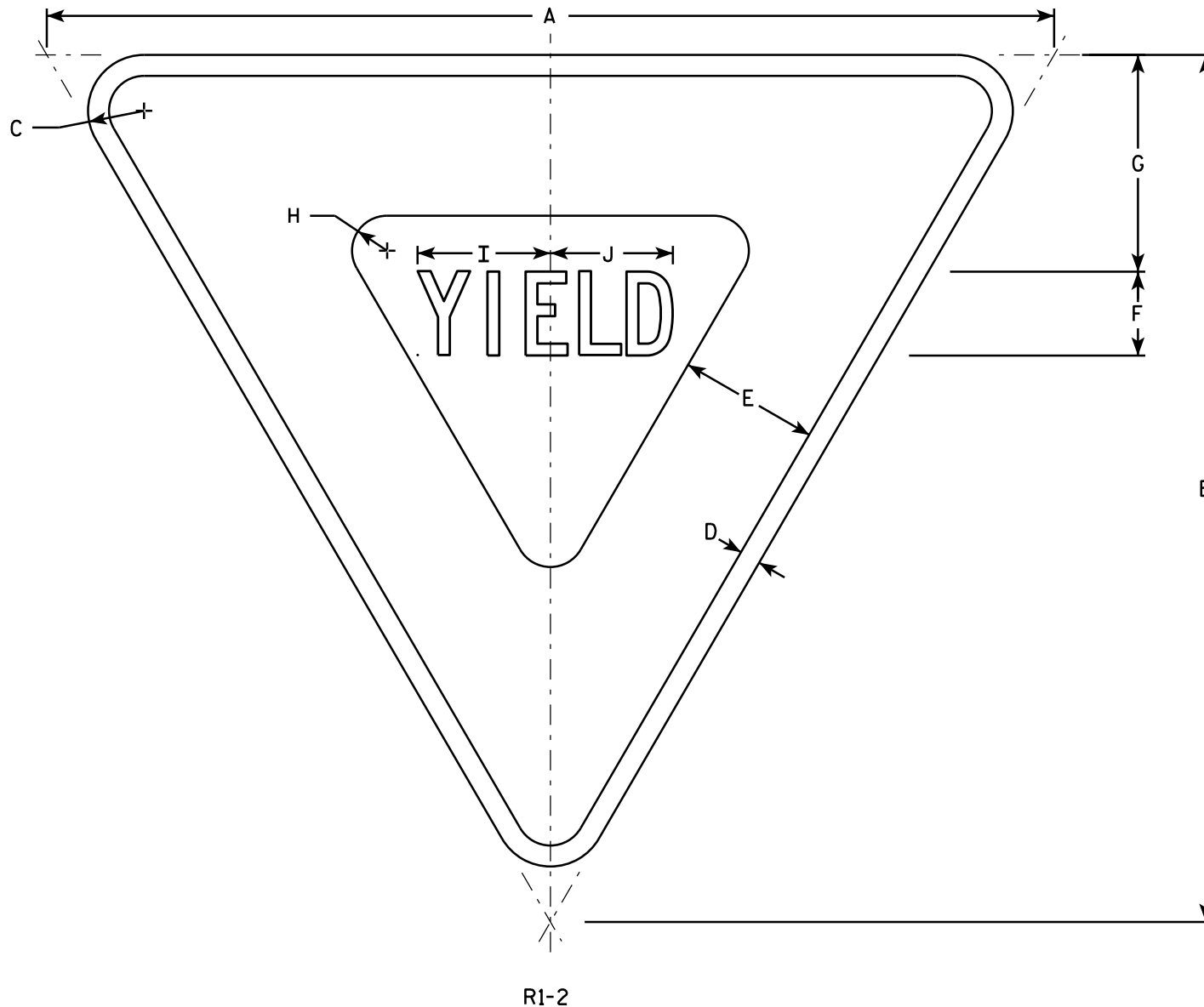
E

STANDARD SIGN  
M6 - 1 & M6 - 2  
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The border strip and word message are reflectorized red.

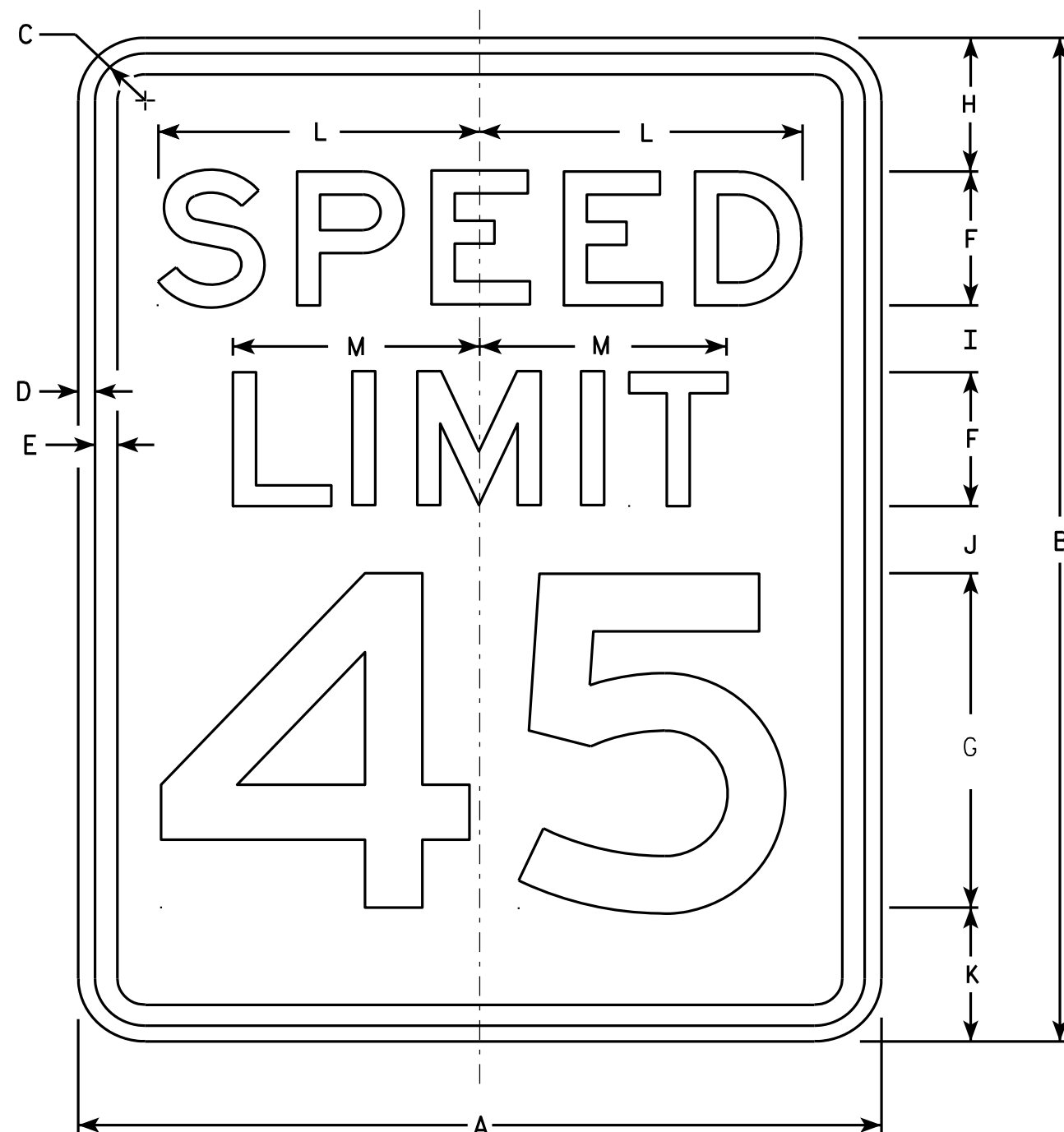
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	26	1 1/2	5/8	4	2 1/2	6 3/8	7/8	4	3 5/8																	2.71
2S	36	31	2	3/4	5	3	7 3/4	1 1/4	4 3/4	4 3/8																	3.88
2M	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
3	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
4	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
5	60	52	3	1 1/2	8	5	13	2 1/2	7 7/8	7 1/4																	10.83
6																											
7	18	15 1/2	1	3/8	2 1/2	1 1/2	3 7/8	5/8	2 3/8	2 1/4																	0.97

STANDARD SIGN  
R1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 10/13/14 PLATE NO. R1-2.12



R2-1

### NOTES

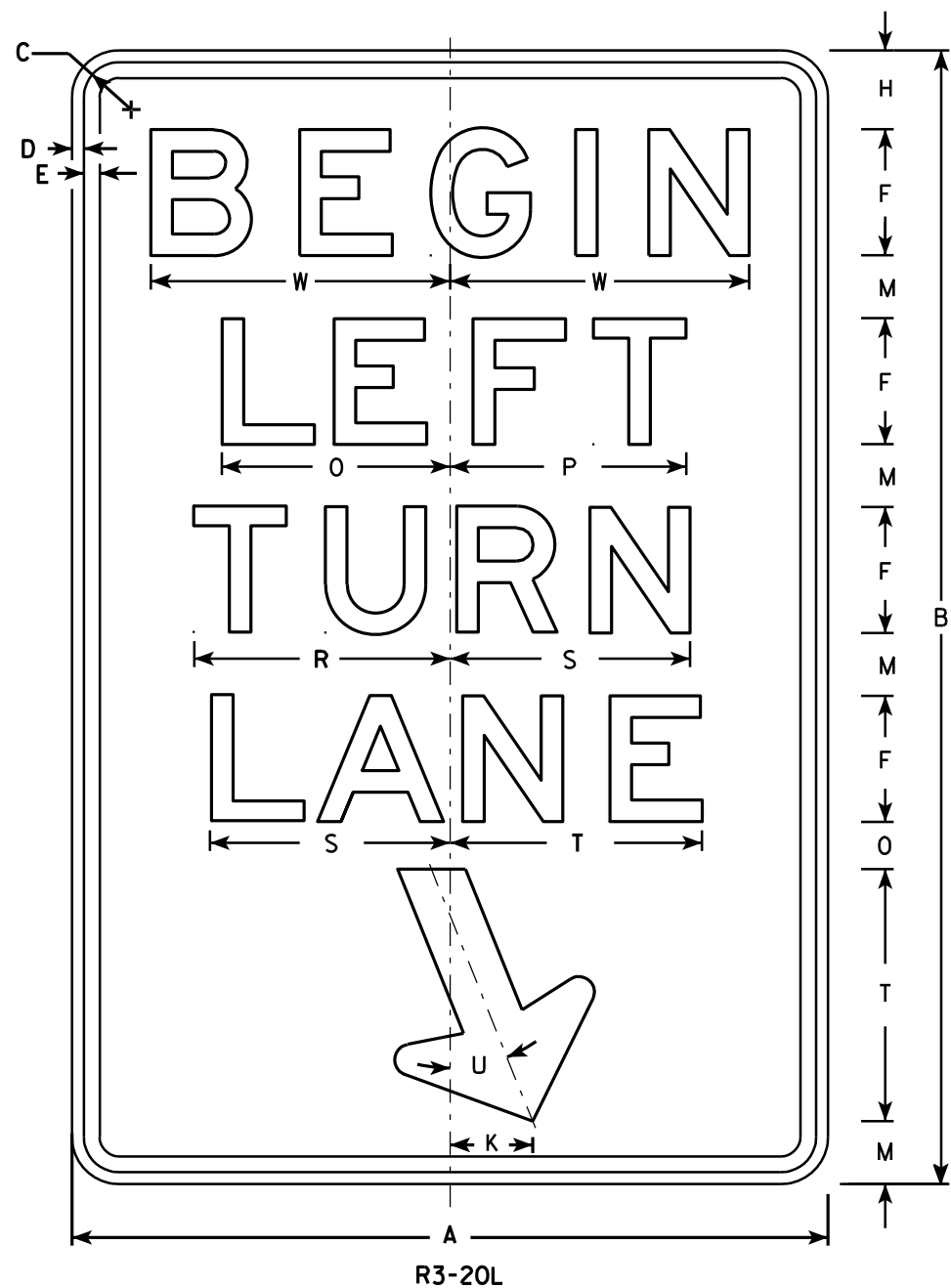
1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

### STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION  
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer  
DATE 5/26/10 PLATE NO. R2-1.13

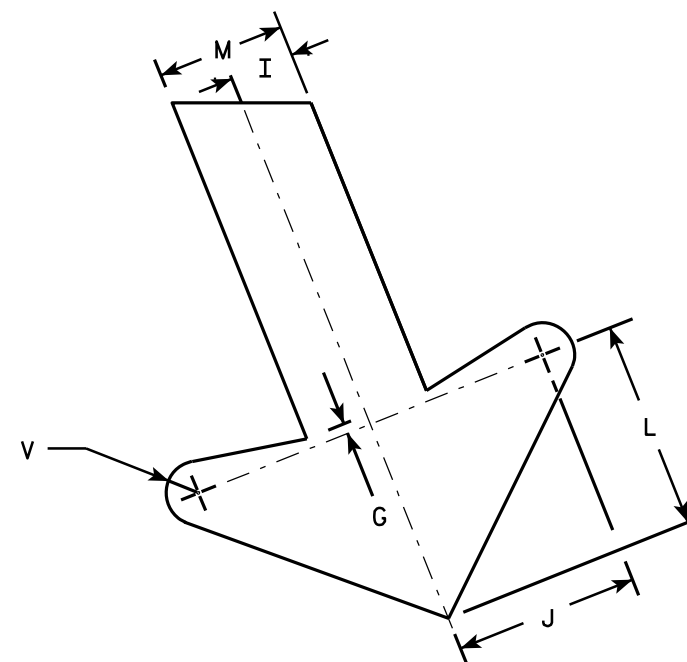
PROJECT NO: HWY: COUNTY: SHEET NO: E



R3-20L

NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:  
Background - White  
Message - Black
- 3. Message Series - E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

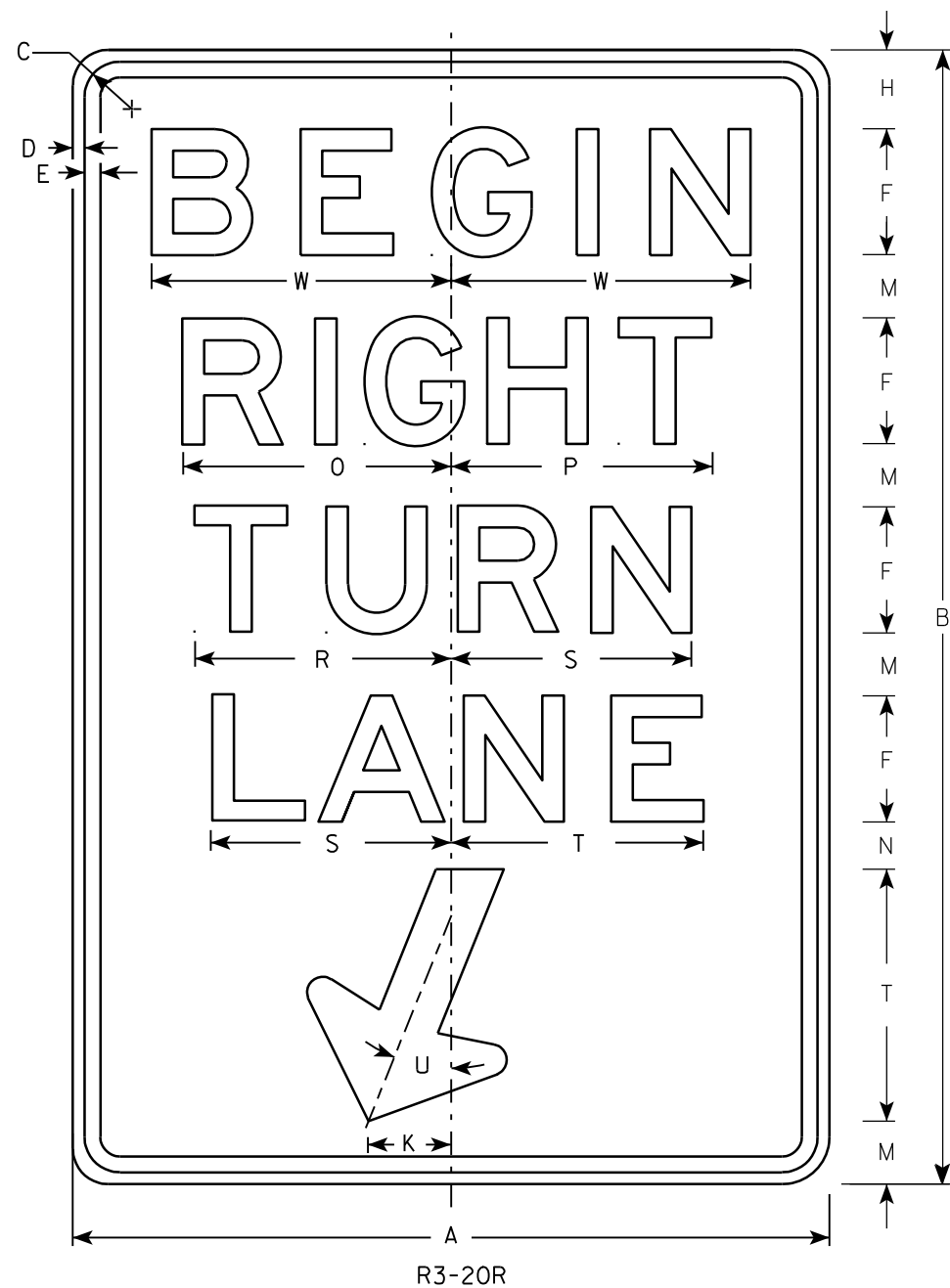
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	7 1/4	7 1/2		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	10 7/8	11 1/4		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

STANDARD SIGN  
R3-20L

WISCONSIN DEPT OF TRANSPORTATION

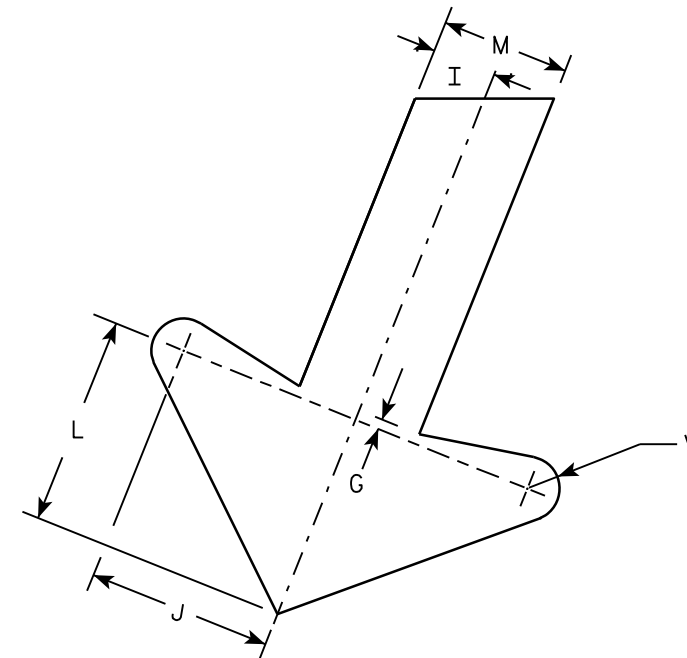
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 10/18/10 PLATE NO. R3-20L.7



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

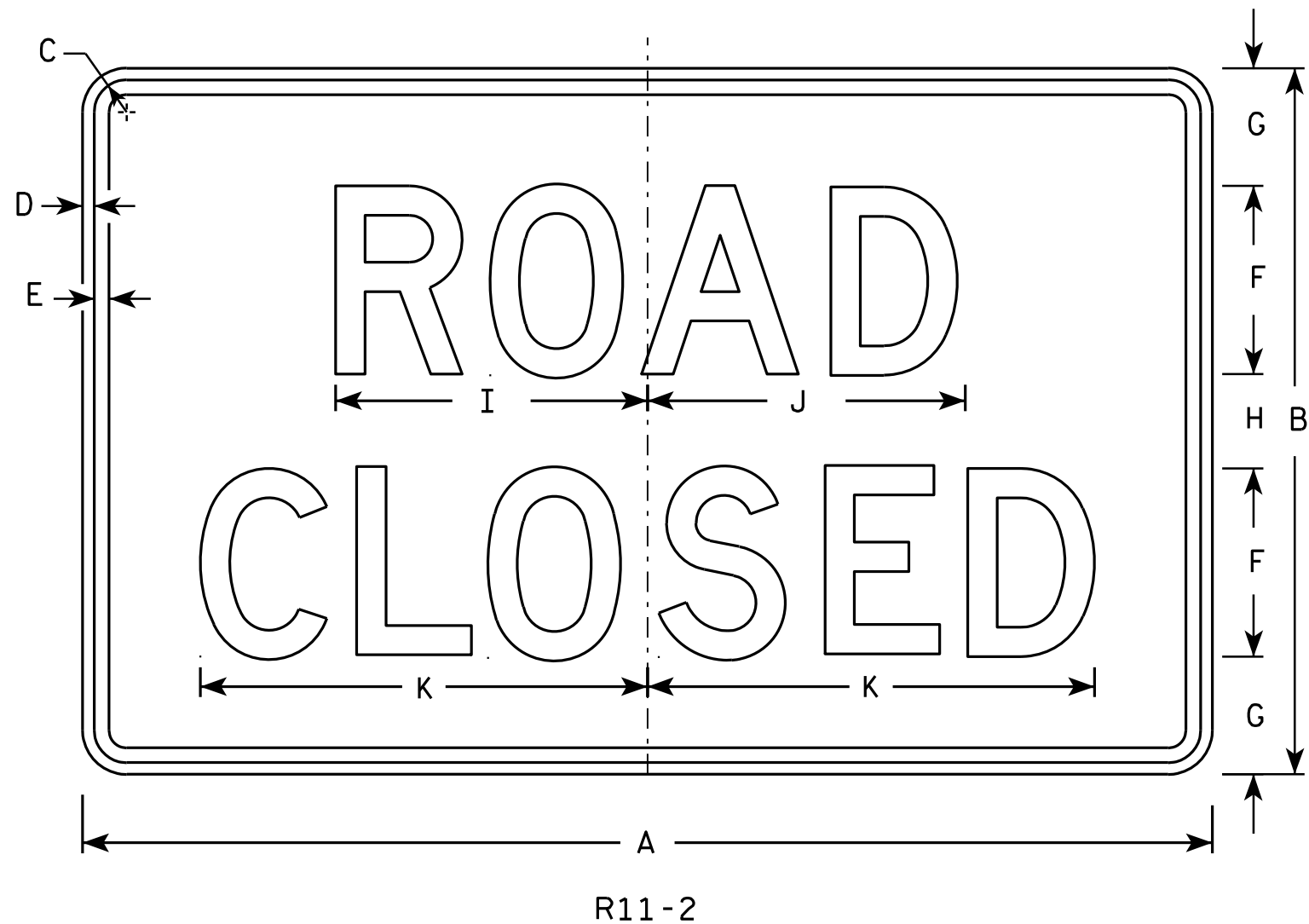


ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	12 3/4	12 1/2		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

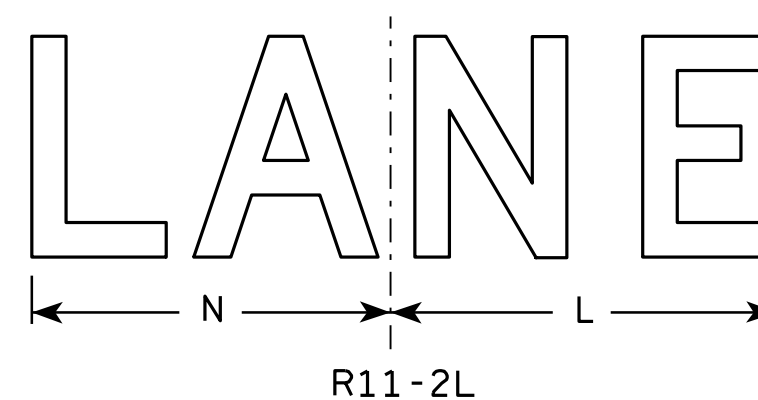
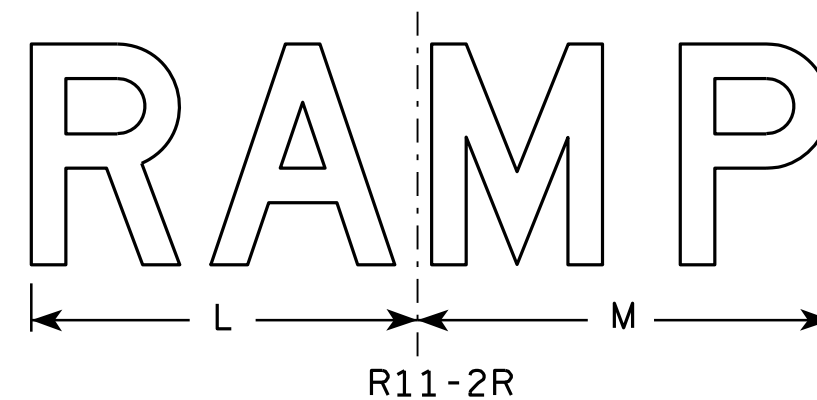
STANDARD SIGN R3-20R	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 10/18/10	PLATE NO. R3-20R.6





### NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

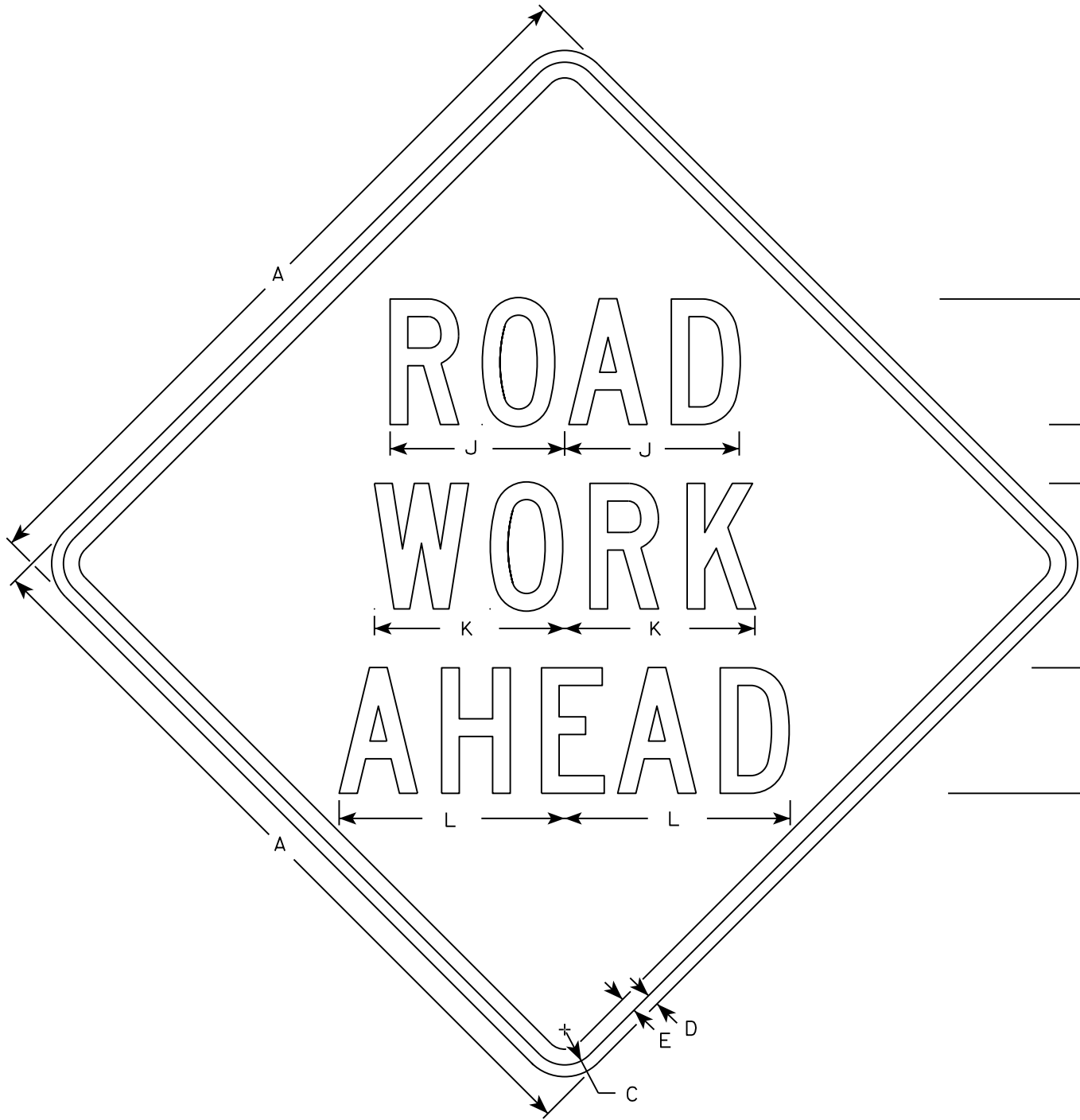


SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
2M	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
3	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
4	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0
5	48	30	1 3⁄8	1⁄2	5⁄8	8	5	4	13 1⁄4	13 1⁄2	19	14	15	13													10.0

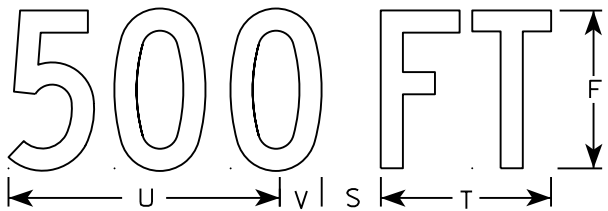
### STANDARD SIGN R11-2

WISCONSIN DEPT OF TRANSPORTATION  
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer  
DATE 4/1/11 PLATE NO. R11-2.10

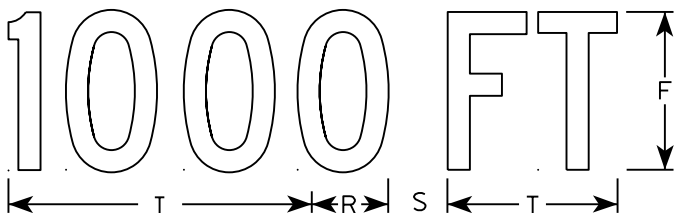
PROJECT NO: HWY: COUNTY: SHEET NO: E



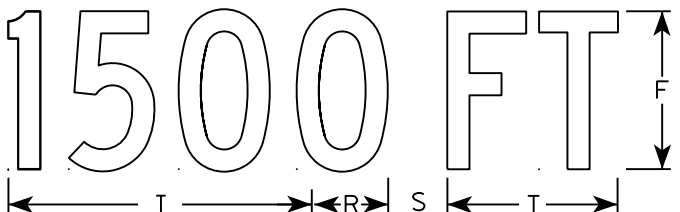
W20-1A



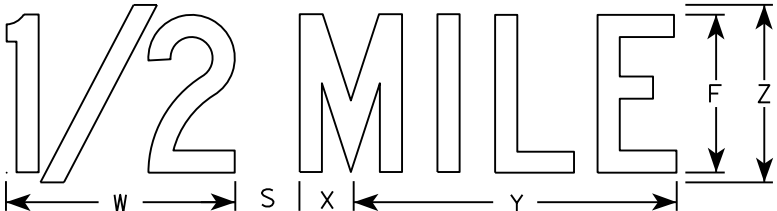
W20-1D



W20-1C



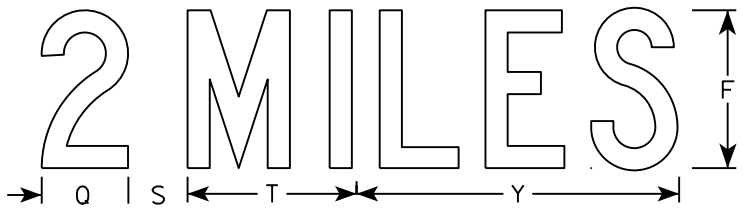
W20-1B



W20-1G



W20-1F



W20-1E

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:  
Background - Orange  
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN  
W20-1A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

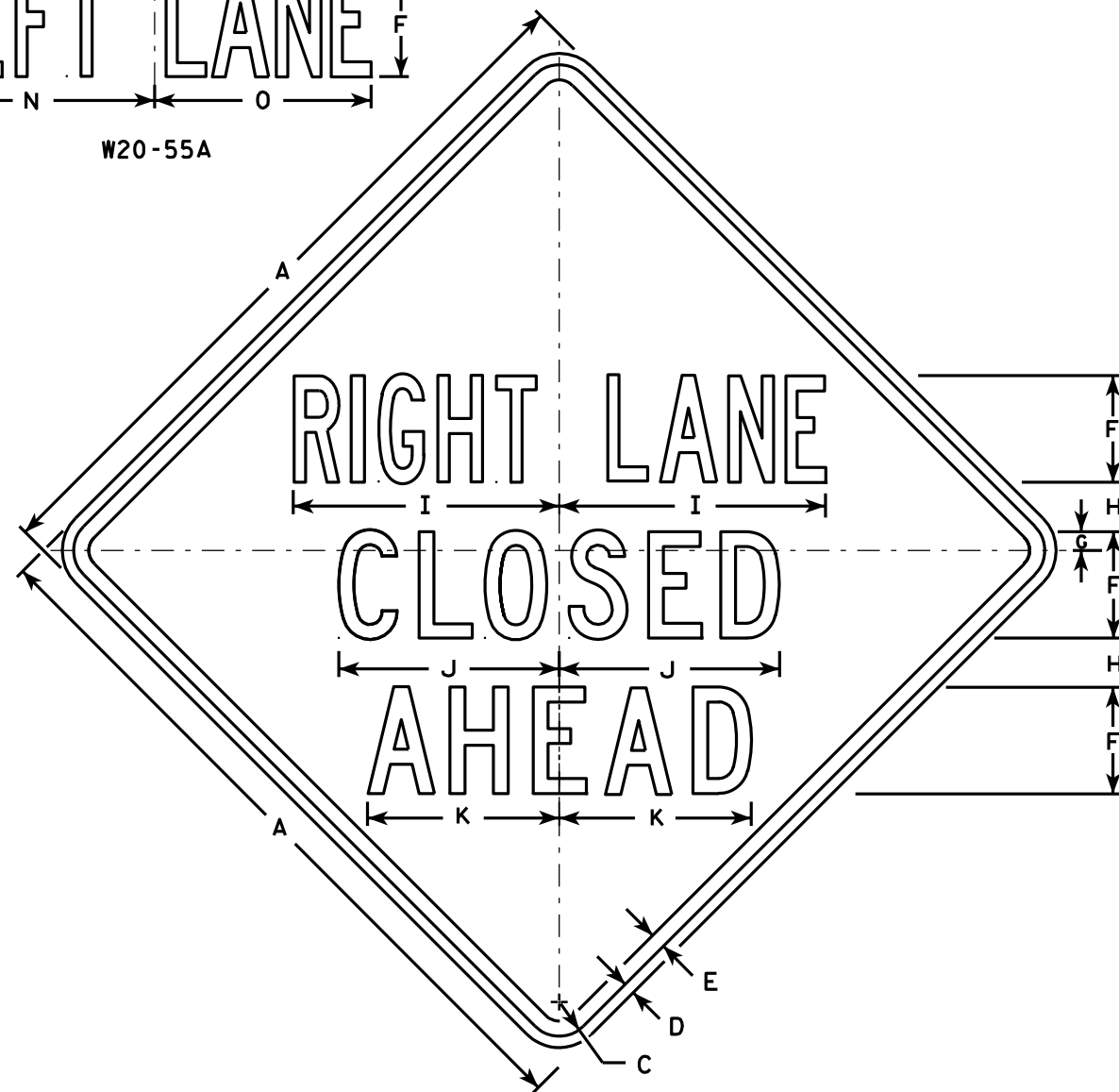
DATE 5/07/15 PLATE NO. W20-1.10

CENTER LANE

W20-56A

LEFT LANE

W20-55A



W20-5A

500 FT

W20-5D

1000 FT

W20-5C

1500 FT

W20-5B

1/2 MILE

W20-5G

1 MILE

W20-5F

### NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. "-----LANE" is Series B.  
All other copy is Series C.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	7/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 7/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

PROJECT NO:

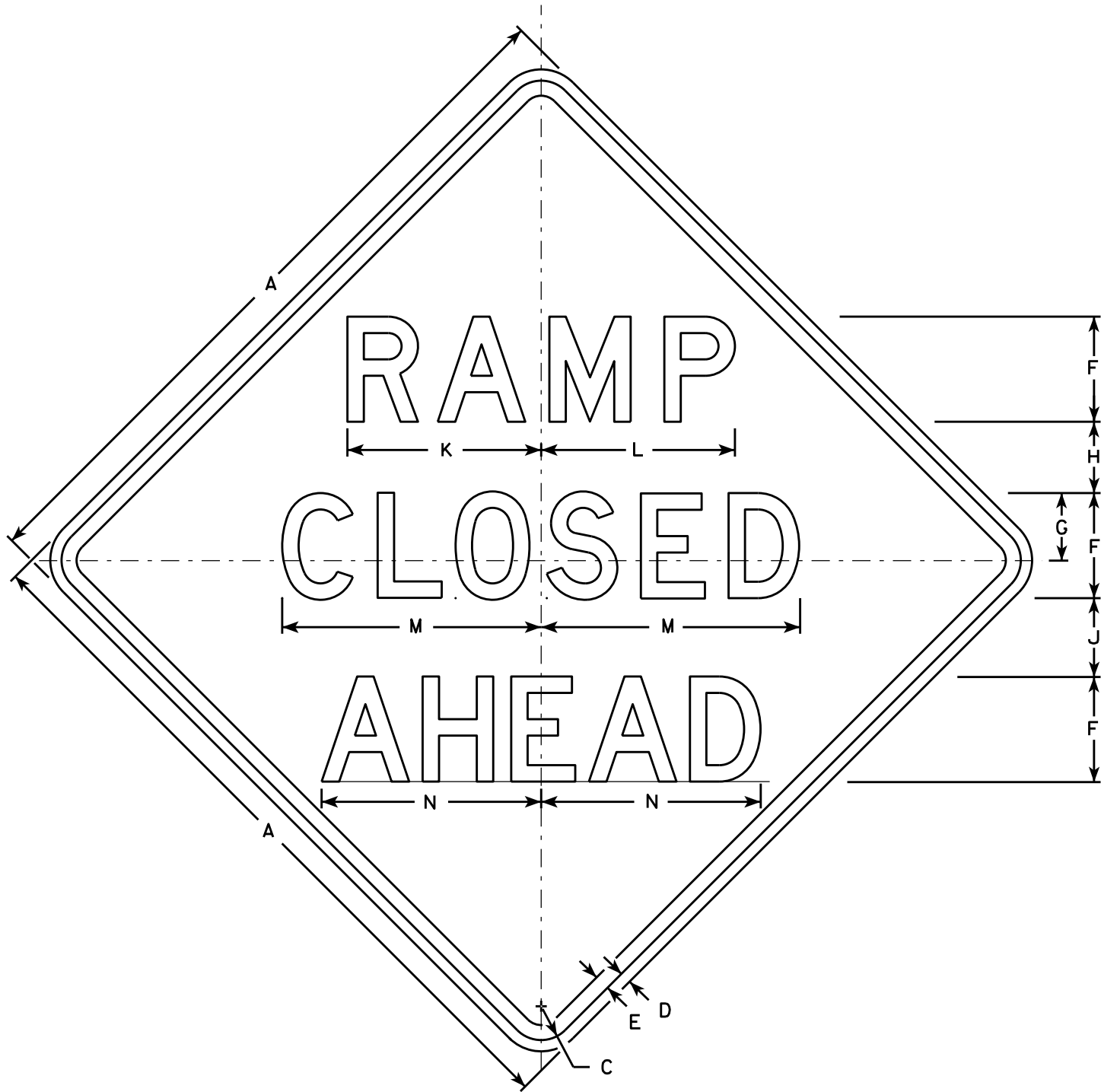
HWY:

COUNTY:

SHEET NO:

E

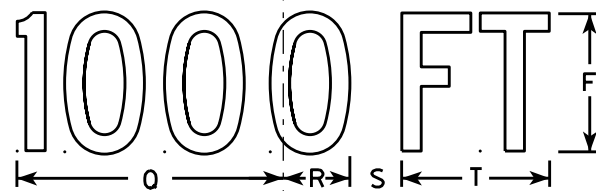
STANDARD SIGN	
W20-5A, B, C, D, F & G	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 3/18/11	PLATE NO. W20-5.11



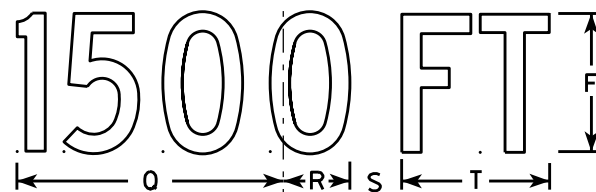
W20-53A



W20-53D



W20-53C



W20-53B



W20-53G



W20-53F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.  
Line 3 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	9 1/4	9 1/4	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	12 7/8	12 7/8	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN  
W20-53A,B,C,D,F,G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/27/15 PLATE NO. W20-53.1

PROJECT NO:

HWY:

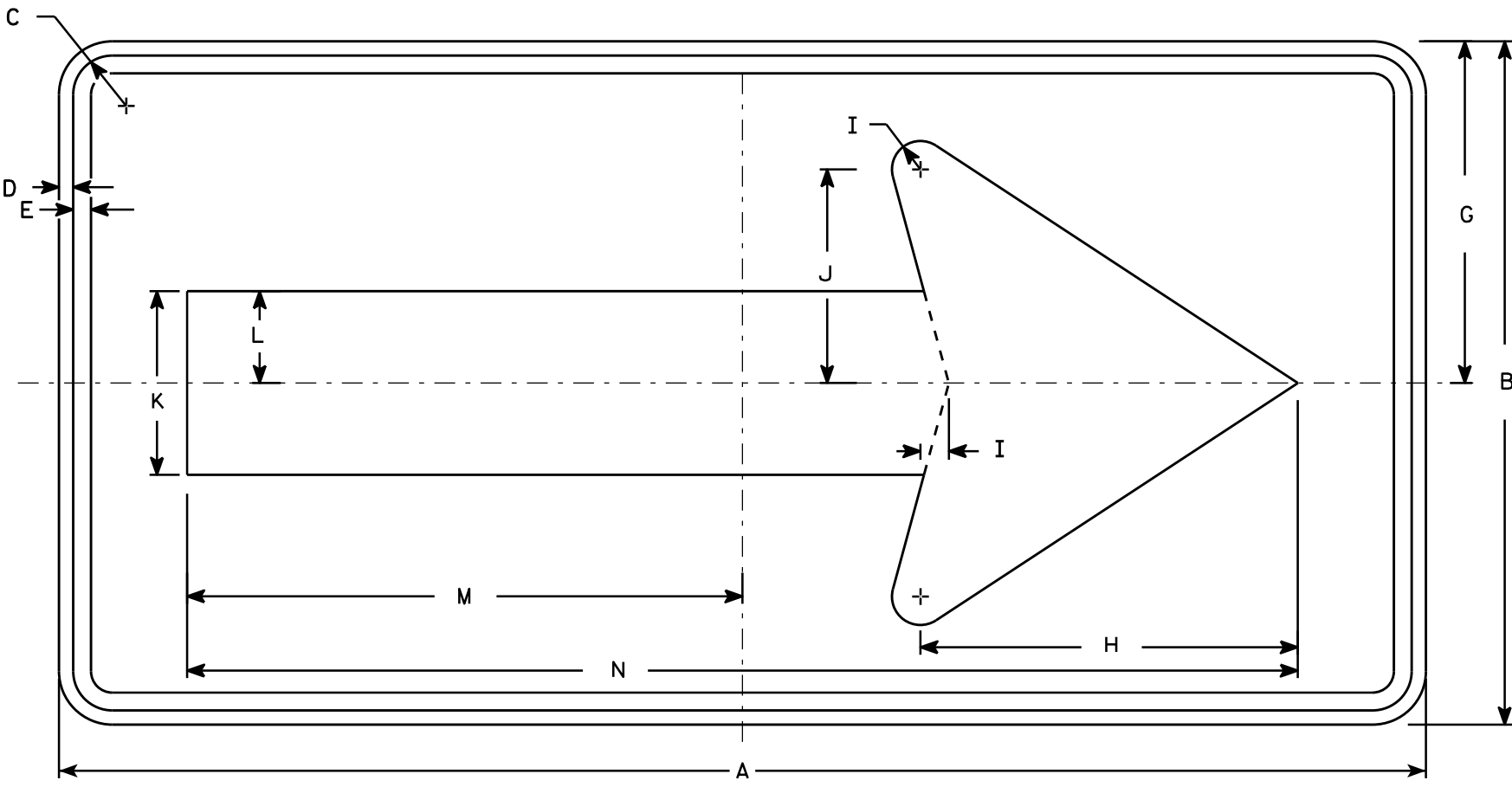
COUNTY:

SHEET NO:

E

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:  
Background - Orange  
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W01-6

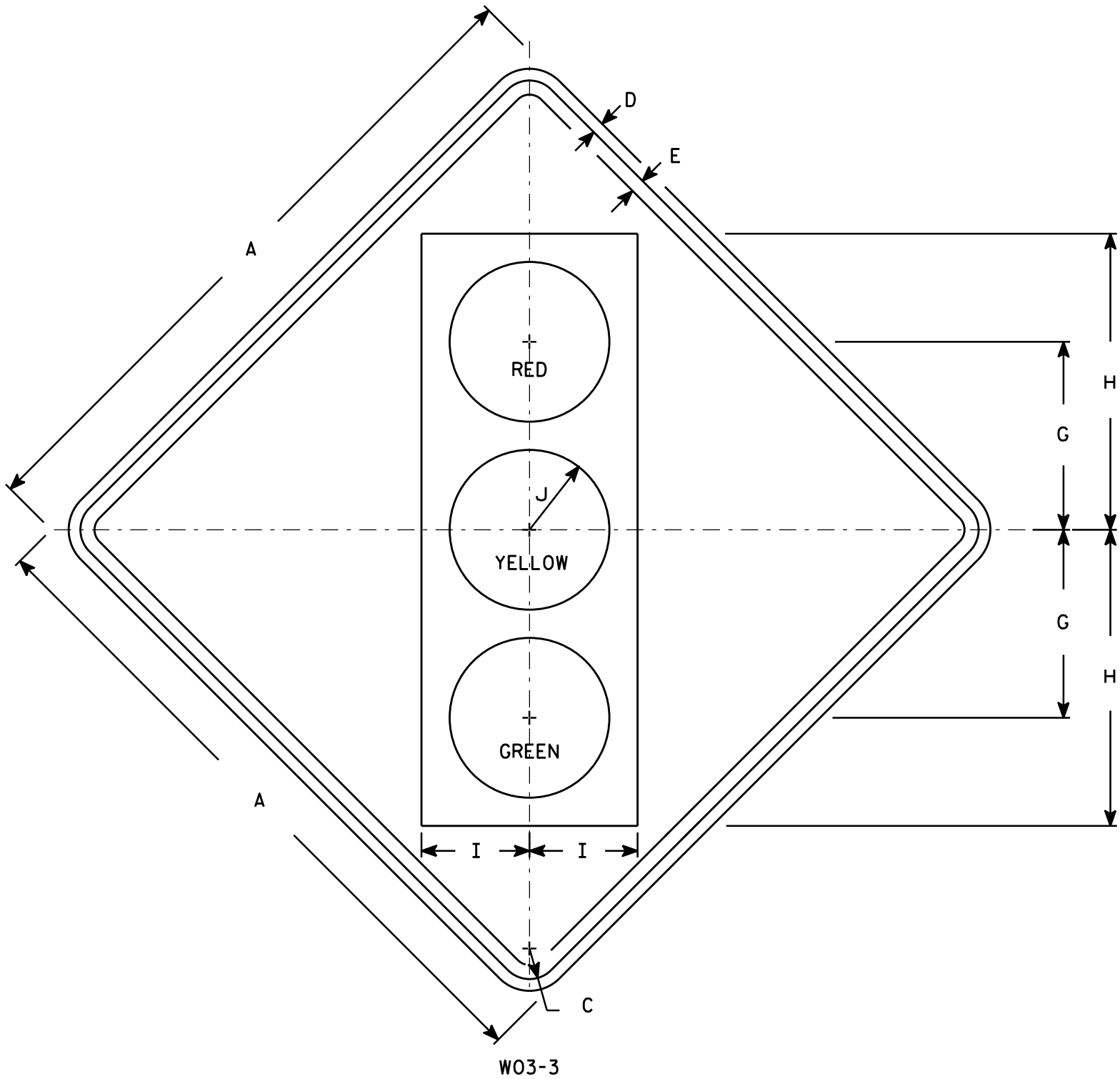
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
2M	48	24	1 3⁄8	1⁄2	5⁄8		12	13 1⁄4	1	7 1⁄2	6 1⁄2	3 1⁄4	19 1⁄2	39													8.0
3	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
4	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5
5	60	30	1 3⁄8	1⁄2	5⁄8		15	16 1⁄4	1 1⁄4	9 1⁄4	8	4	24 3⁄8	48 3⁄4													12.5

STANDARD SIGN  
W01-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-6.1



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - See Note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Symbol and border are non-reflective black.  
Top circle - Type H ReflectORIZED Red  
Center circle - Same as background  
Bottom circle - Type H ReflectORIZED Green

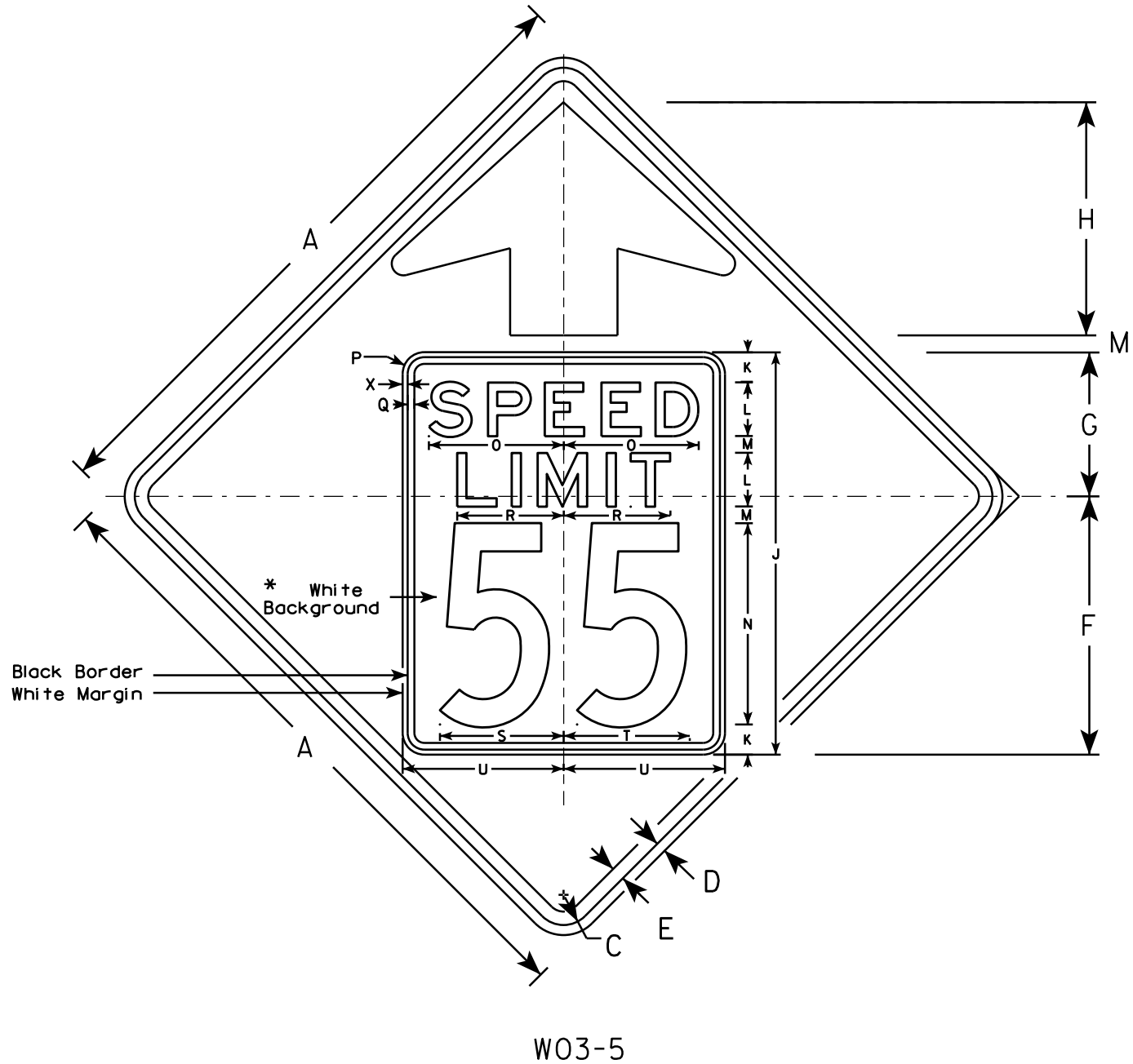
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4		10	15 3/4	5 3/4	4 1/4																	9.0
2S	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
2M	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
3	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
4	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
5	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0

STANDARD SIGN  
W03-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

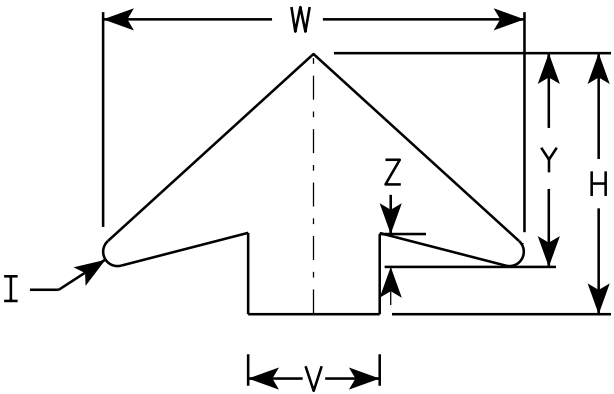
DATE 11/20/13 PLATE NO. W03-3.1



NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color: \*  
Background - ORANGE\*  
Message - BLACK
- 3. Message Series - C for numbers Series E for wording
- 4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

\*Speed Limit Sign shall have a White Background



ARROW DETAIL

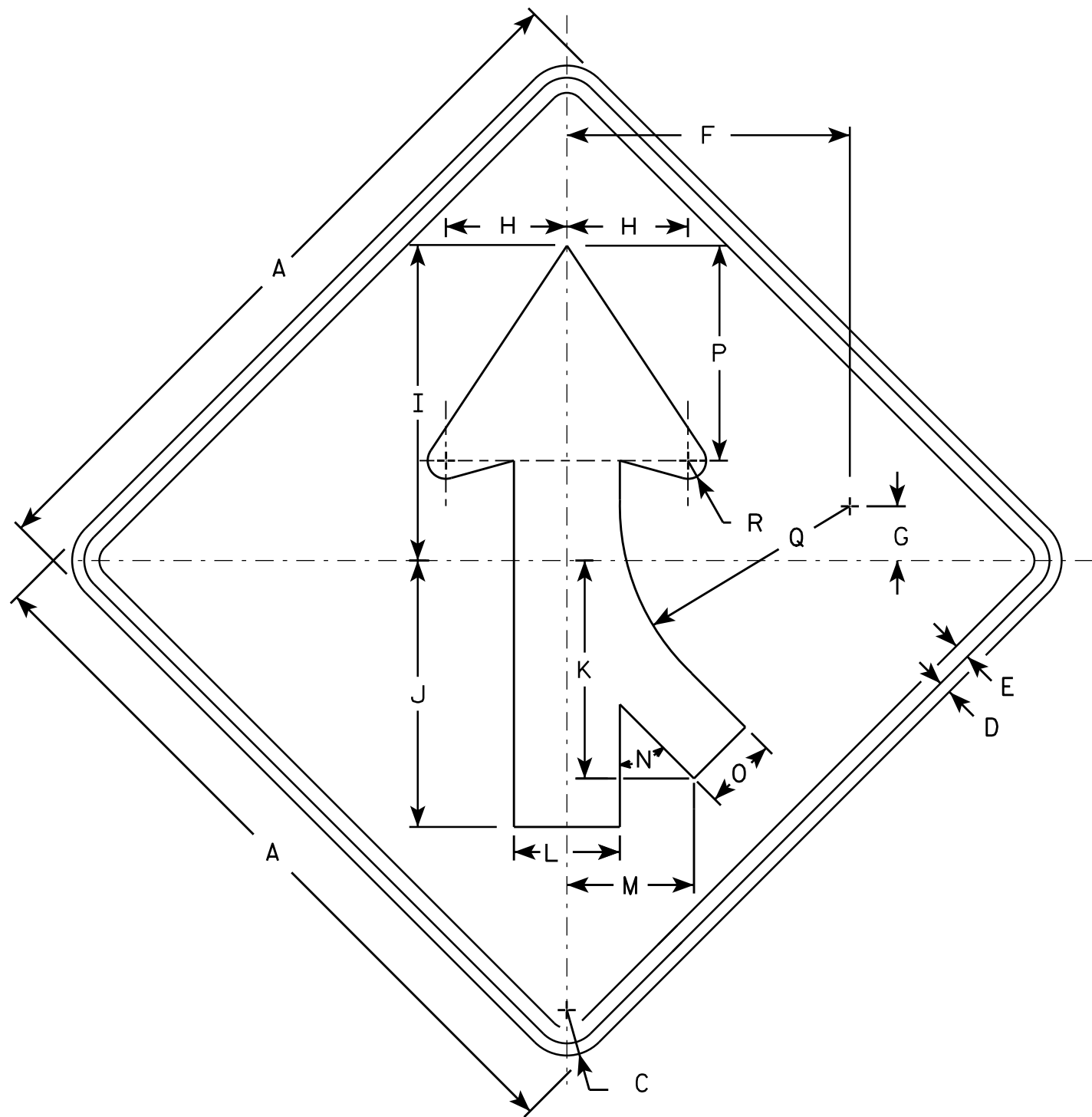
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
2S	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0
2M	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0
3	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0
4	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0
5	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0

STANDARD SIGN  
W03-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/20/13 PLATE NO. W03-5.1



W04-1R

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:  
Background - Orange  
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W4-1L is the same as W4-1R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	14	2 3/4	6	15 3/4	13 1/4	10 1/4	5 1/4	6 3/8	45°	3 5/8	10 5/8	11 3/8	7/8									9.0
2S	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4									16.0
2M	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4									16.0
3	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4									16.0
4	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4									16.0
5	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4									16.0

STANDARD SIGN

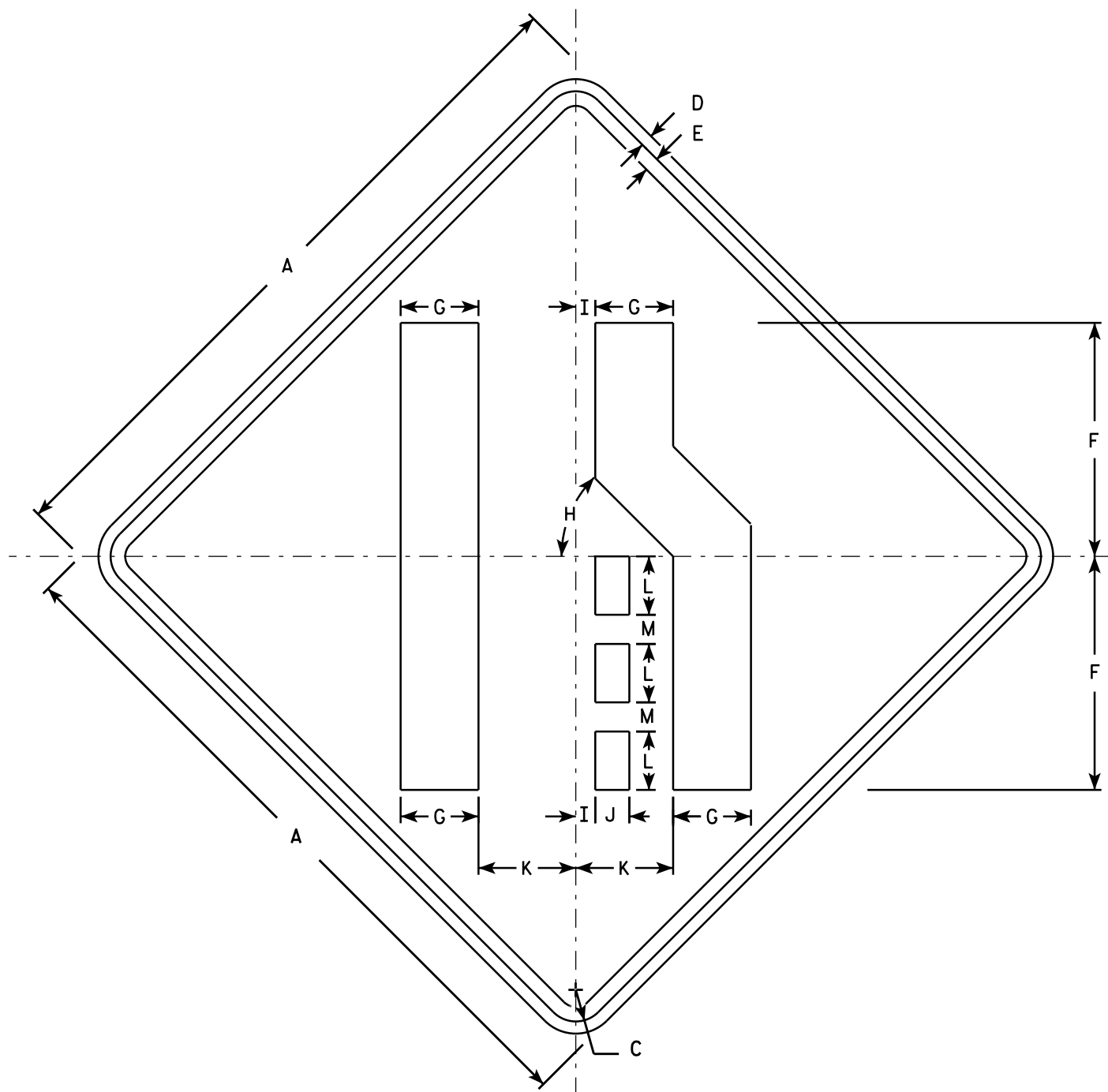
W04-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/20/13 PLATE NO. W04-1.1





W04-2R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W04-2L is the same as W04-2R except the symbol is reversed along the vertical centerline.

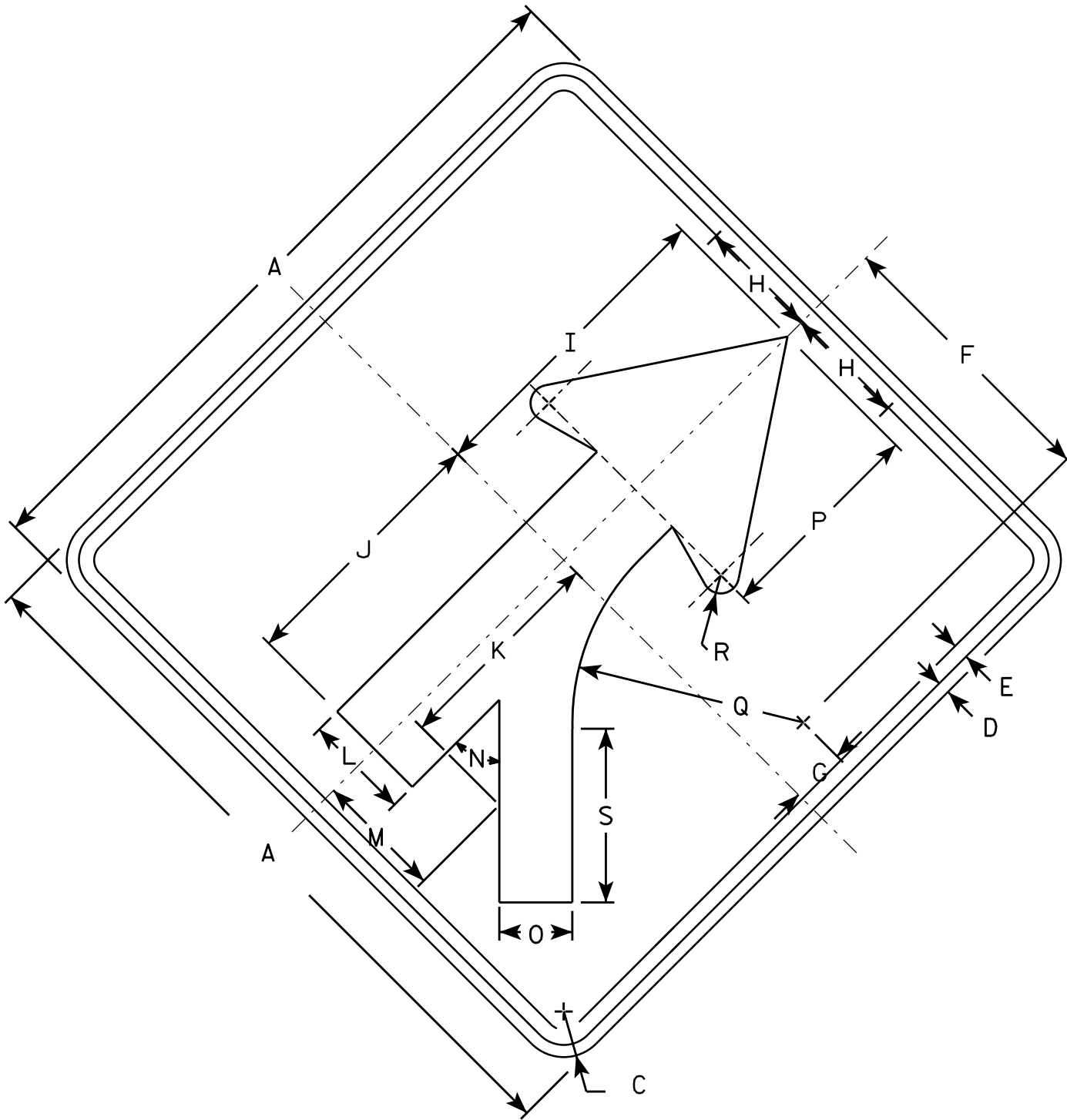
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
2M	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
3	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
4	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0

STANDARD SIGN  
W04-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
For State Traffic Engineer

DATE 11/20/13 PLATE NO. W04-2.1



W04-5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	14	2 3/4	6	15 3/4	13 1/4	10 1/4	5 1/4	6 3/8	45°	3 5/8	10 5/8	11 3/8	7/8	8 1/2								9.0
2S	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4	11 3/8								16.0
2M	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4	11 3/8								16.0
3	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4	11 3/8								16.0
4	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4	11 3/8								16.0
5	48		2 1/4	3/4	1	18 3/4	3 5/8	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4	11 3/8								16.0

NOTES

1. Sign is Type II - Type F Reflective - reference  
WIS DOT Standard Specification for HIGHWAY  
and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Orange  
Message - Black
3. Corners may be square or rounded when base  
material is plywood but borders shall be rounded  
as shown. When base material is metal, the  
corners and borders shall be rounded.

STANDARD SIGN  
W04-5

WISCONSIN DEPT OF TRANSPORTATION

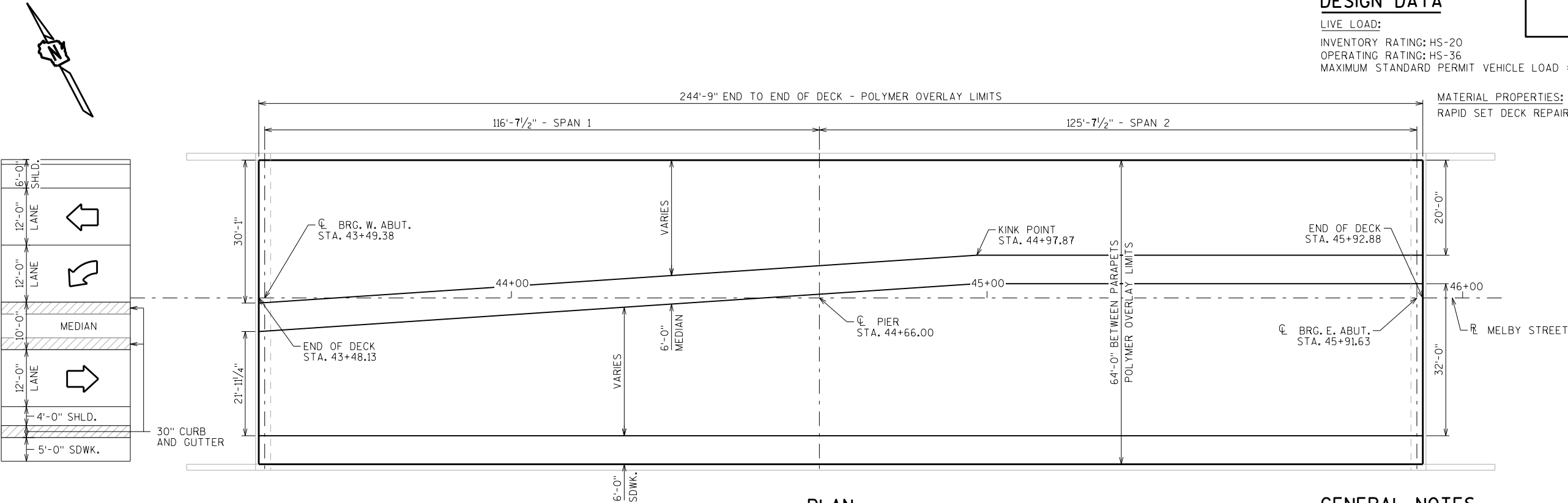
APPROVED *Matthew P. Rauch*  
for State Traffic Engineer

DATE 4/28/16 PLATE NO. W04-5.1

DESIGN DATA

LIVE LOAD:  
INVENTORY RATING: HS-20  
OPERATING RATING: HS-36  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR -  $f'c$  = 4,000 P.S.I.

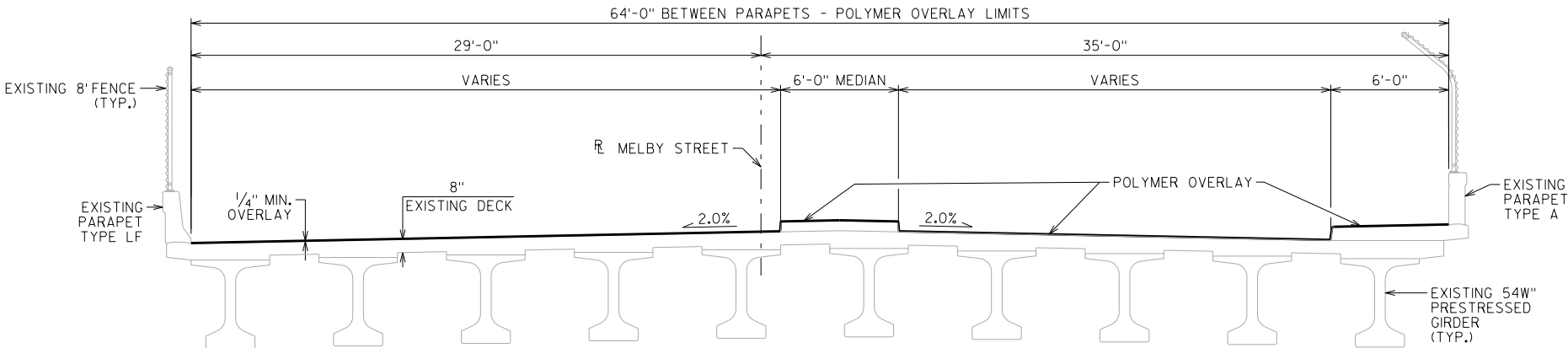


PLAN

2 SPAN 54W" PRESTRESSED GIRDER BRIDGE

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".  
AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.  
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".  
OVERLAY ON THE SIDES AND TOP OF THE MEDIAN AND SIDEWALK IS INCLUDED IN THE QUANTITY FOR BID ITEM "POLYMER OVERLAY".




CROSS SECTION THRU ROADWAY

LOOKING UPSTATION

TOTAL ESTIMATED QUANTITIES

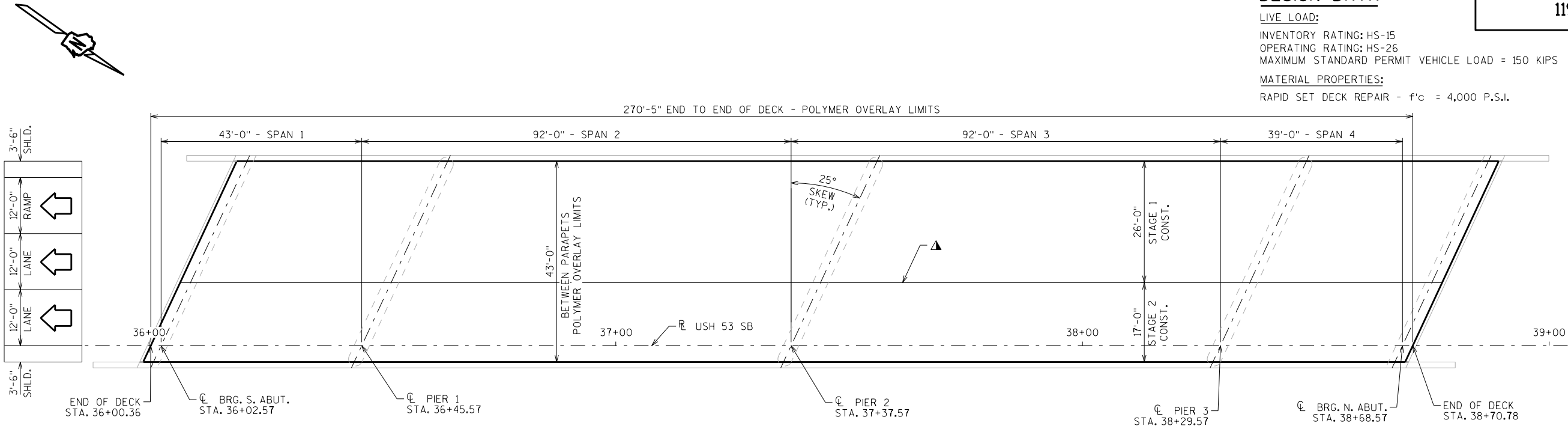
BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	4
509.0302	PREPARATION DECKS TYPE 2	SY	2
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	40
509.5100.S	POLYMER OVERLAY	SY	1786
SPV.0035	RAPID SET DECK REPAIR	CY	1

STRUCTURE DESIGN CONTACTS:  
MICHAEL LARSON (608) 267-4539  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
 <b>BUREAU OF STRUCTURES</b>			
ACCEPTED <i>William C. Decker</i>		8/3/18	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
<b>STRUCTURE B-9-262</b>			
MELBY STREET OVER USH 53			
COUNTY	CHIPPEWA	TOWN/CITY/VILLAGE	HALLIE
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	MJL	DESIGNED CK'D.	DLM
DRAWN BY	MJL	PLANS CK'D.	DLM
<b>POLYMER OVERLAY</b>			SHEET 1 OF 1

DESIGN DATA

LIVE LOAD:  
INVENTORY RATING: HS-15  
OPERATING RATING: HS-26  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 150 KIPS  
MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR - f'c = 4,000 P.S.I.



PLAN

4 SPAN STEEL GIRDER BRIDGE

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

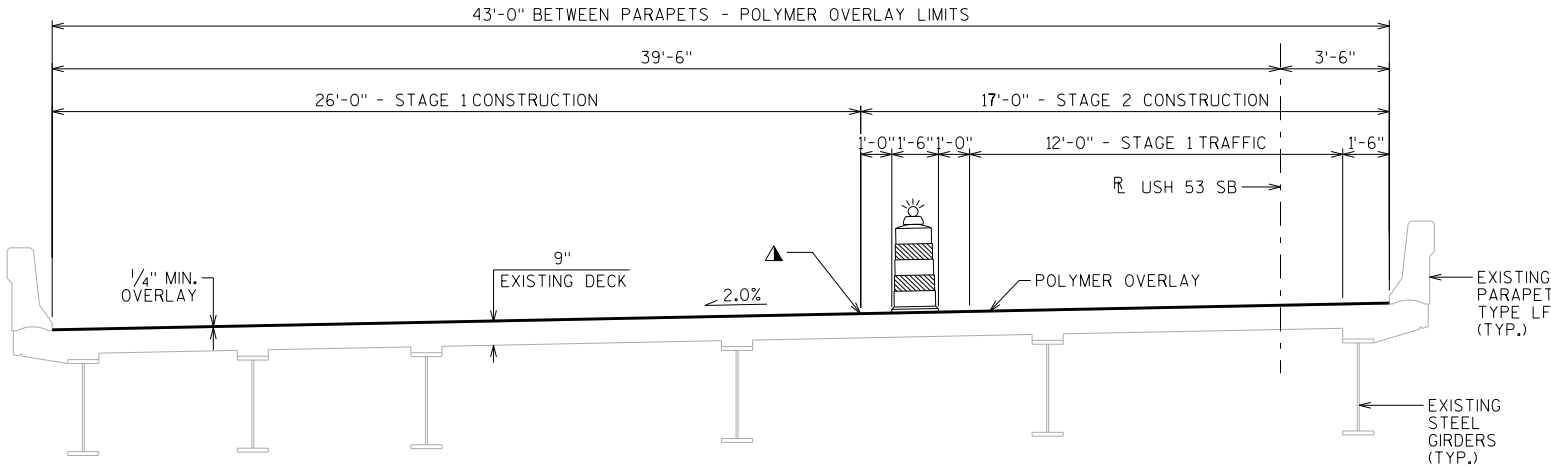
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.

"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER, DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".

▲ LONGITUDINAL CONSTRUCTION JOINT




CROSS SECTION THRU ROADWAY

LOOKING UPSTATION  
(STAGE 1 TRAFFIC SHOWN, STAGE 2 IS SIMILAR)

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	1292
SPV.0035	RAPID SET DECK REPAIR	CY	1

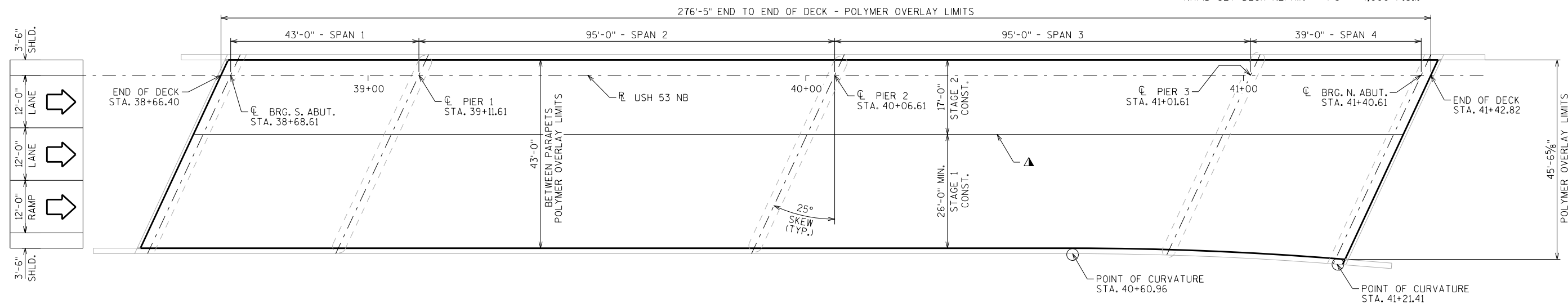
STRUCTURE DESIGN CONTACTS:  
MICHAEL LARSON (608) 267-4539  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
 <b>BUREAU OF STRUCTURES</b>			
ACCEPTED <i>William C. Dreher</i> <sup>LLS</sup>		8/3/18	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
<b>STRUCTURE B-18-35</b>			
USH 53 SB OVER IH 94			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	WASHINGTON
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	MJL	DESIGNED CK'D.	DLM
DRAWN BY	MJL	PLANS CK'D.	DLM
<b>POLYMER OVERLAY</b>			SHEET 1 OF 1

LIVE LOAD:

INVENTORY RATING: HS-13  
OPERATING RATING: HS-21  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 130 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR - f'c = 4,000 P.S.I.



#### 4 SPAN STEEL GIRDER BRIDGE

DRAWINGS SHALL NOT BE SCALED.

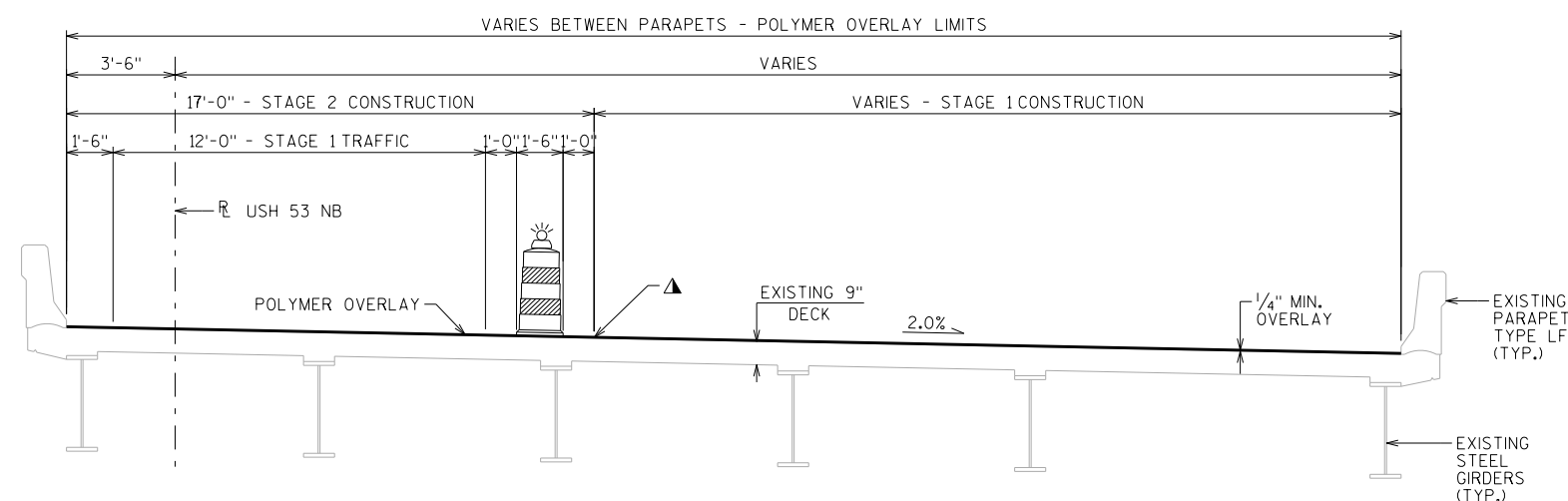
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.

"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".


▲ LONGITUDINAL CONSTRUCTION JOINT



LOOKING UPSTATION  
(STAGE 1 TRAFFIC SHOWN, STAGE 2 IS SIMILAR)

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	1,327
SPV.0035	RAPID SET DECK REPAIR	CY	1

STRUCTURE DESIGN CONTACTS:  
MICHAEL LARSON (608) 267-4539  
LAURA SHADEWALD (608) 267-9592

NO.		DATE		REVISION		BY	
				<b>BUREAU OF</b> <b>STRUCTURES</b>			
ACCEPTED		<i>William C. Diehn</i> <sup>LES</sup>		8/3/18			
CHIEF STRUCTURES DESIGN ENGINEER				DATE			
STRUCTURE B-18-36							
USH 53 NB OVER IH 94							
COUNTY EAU CLAIRE				TOWN/CITY/VILLAGE WASHINGTON			
DESIGN SPEC. REHABILITATION N/A							
DESIGNED BY MJL		DESIGNED CK'D. DLM		DRAWN BY		PLANS CK'D. DLM	
POLYMER OVERLAY				SHEET 1 OF 1			

DESIGN DATA

LIVE LOAD:  
INVENTORY RATING: HS-17  
OPERATING RATING: HS-29  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 170 KIPS  
  
MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR - f'c = 4,000 P.S.I.

GENERAL NOTES


DRAWINGS SHALL NOT BE SCALED.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".  
AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.  
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".  
OVERLAY ON THE SIDES AND TOP OF THE MEDIAN IS INCLUDED IN THE QUANTITY FOR BID ITEM "POLYMER OVERLAY".

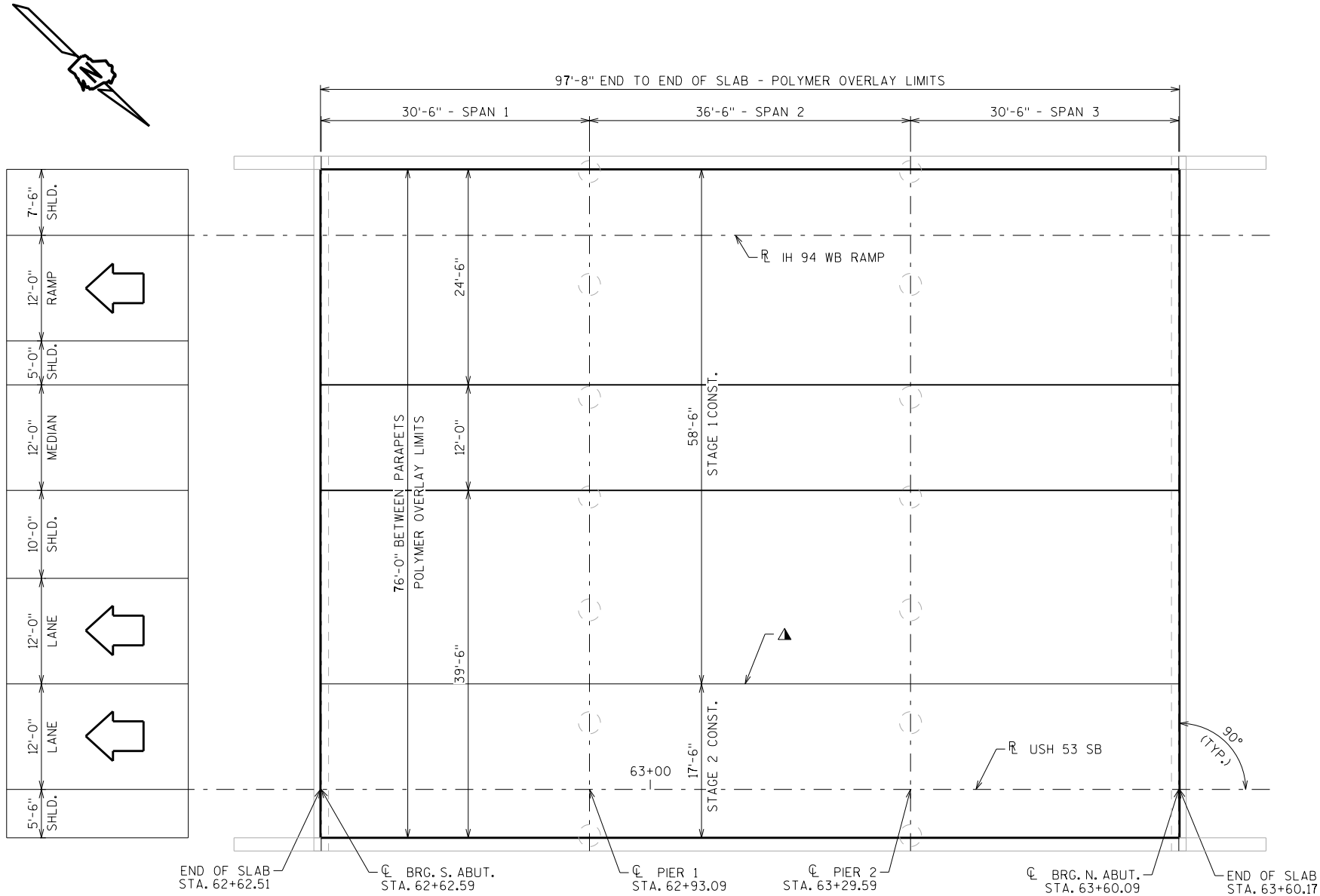
▲ LONGITUDINAL CONSTRUCTION JOINT

TOTAL ESTIMATED QUANTITIES

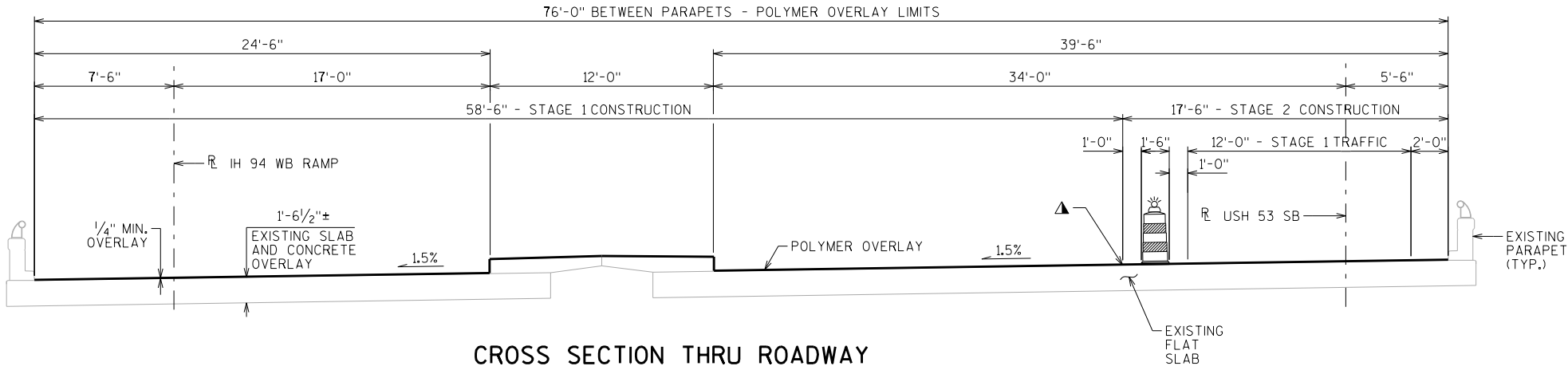
BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	842
SPV.0035	RAPID SET DECK REPAIR	CY	1

STRUCTURE DESIGN CONTACTS:  
MICHAEL LARSON (608) 267-4539  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
		<b>BUREAU OF STRUCTURES</b>	
ACCEPTED <i>William C. Diehn</i> <sup>U.S.</sup>		8/3/18	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-18-37			
USH 53 SB OVER KEYSTONE CROSSING			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	WASHINGTON
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	MJL	DESIGNED CK'D.	DLM
DRAWN BY	MJL	PLANS CK'D.	DLM
POLYMER OVERLAY			SHEET 1 OF 1



PLAN  
3 SPAN FLAT SLAB BRIDGE



CROSS SECTION THRU ROADWAY  
LOOKING UPSTATION  
(STAGE 1 TRAFFIC SHOWN, STAGE 2 IS SIMILAR)

DESIGN DATA

LIVE LOAD:  
INVENTORY RATING: HS-18  
OPERATING RATING: HS-30  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 170 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR -  $f'c$  = 4,000 P.S.I.

GENERAL NOTES


DRAWINGS SHALL NOT BE SCALED.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".  
AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.  
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".

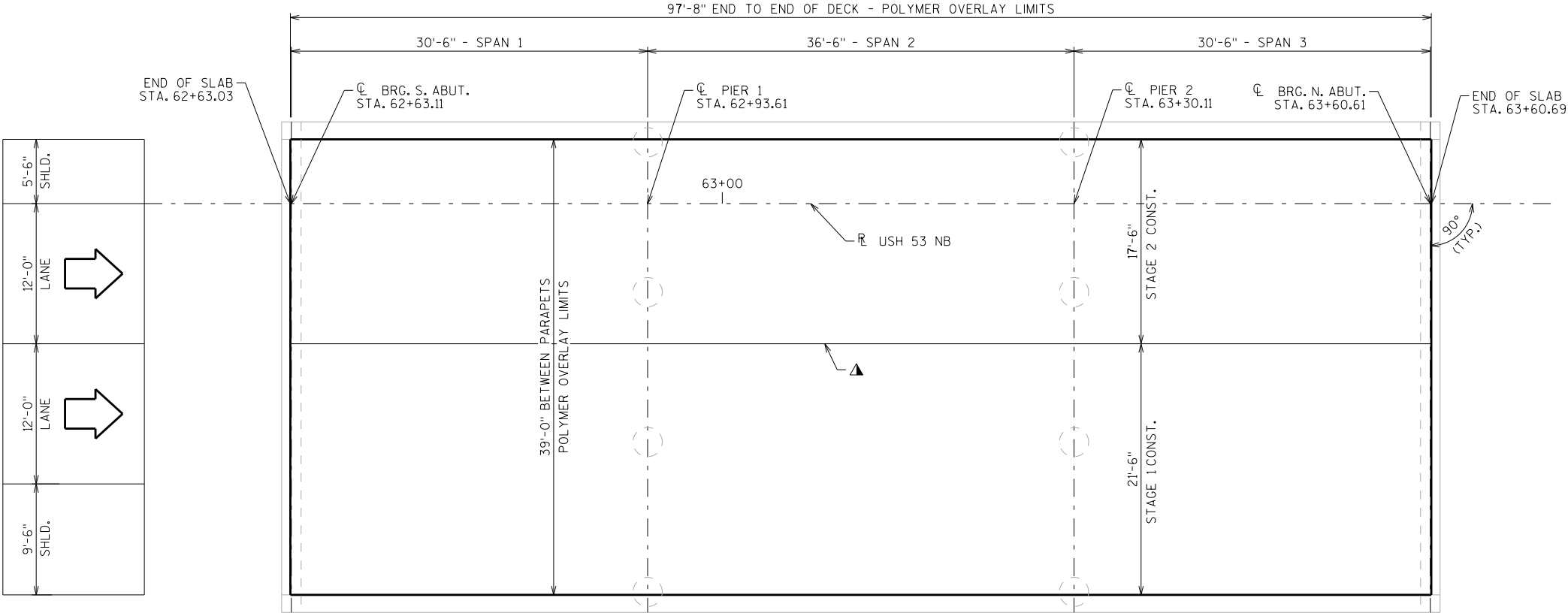
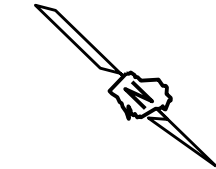
▲ LONGITUDINAL CONSTRUCTION JOINT

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	5
509.0302	PREPARATION DECKS TYPE 2	SY	2
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	50
509.5100.S	POLYMER OVERLAY	SY	424
SPV.0035	RAPID SET DECK REPAIR	CY	1

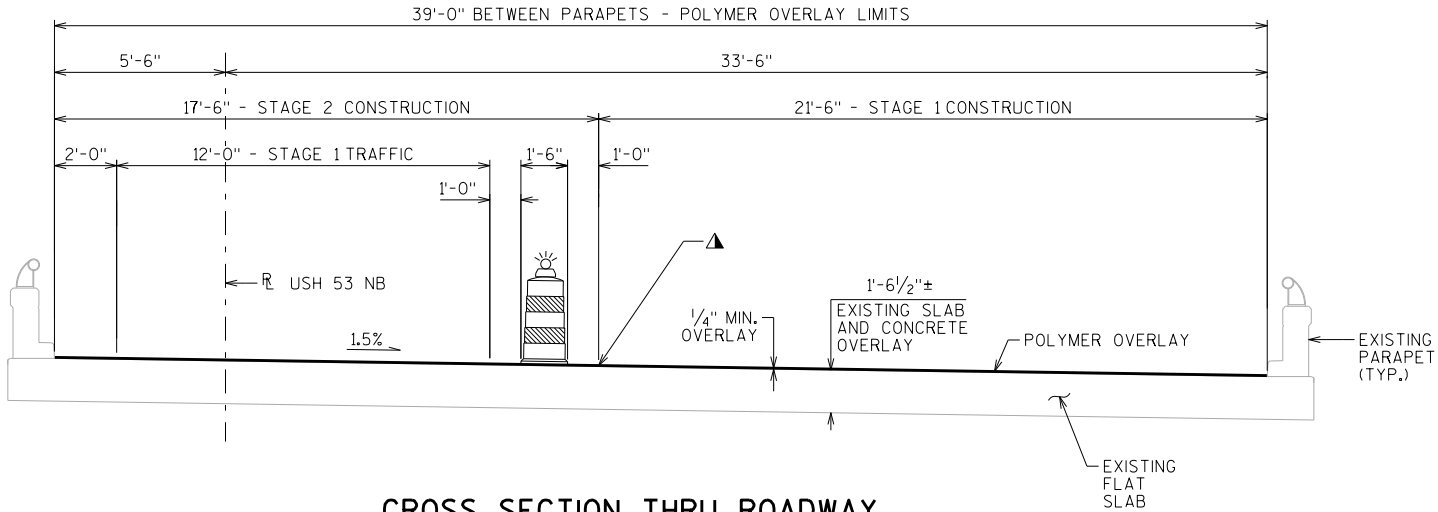
STRUCTURE DESIGN CONTACTS:  
MICHAEL LARSON (608) 267-4539  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
		<b>BUREAU OF STRUCTURES</b>	
ACCEPTED <i>William C. Dreher</i> <sup>LLS</sup>		8/3/18	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
<b>STRUCTURE B-18-38</b>			
USH 53 NB OVER KEYSTONE CROSSING			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	WASHINGTON
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	MJL	DESIGNED CK'D.	DLM
DRAWN BY	MJL	PLANS CK'D.	DLM
<b>POLYMER OVERLAY</b>		SHEET 1 OF 1	



PLAN

3 SPAN FLAT SLAB BRIDGE



CROSS SECTION THRU ROADWAY

LOOKING UPSTATION  
(STAGE 1 TRAFFIC SHOWN, STAGE 2 IS SIMILAR)

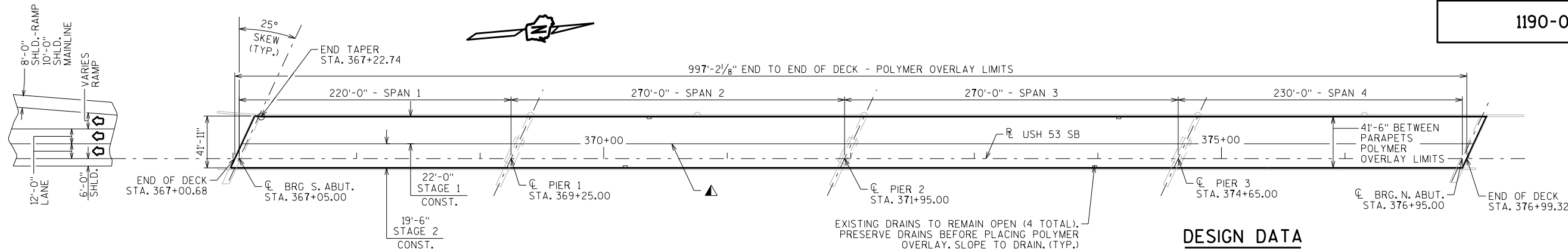
RAPID SET DECK REPAIR —  $f'_c = 4,000$  P.S.I.

2 SPAN - 54W" PRESTRESSED GIRDER BRIDGE

8

8





PLAN

4 SPAN STEEL PLATE GIRDER BRIDGE

DESIGN DATA

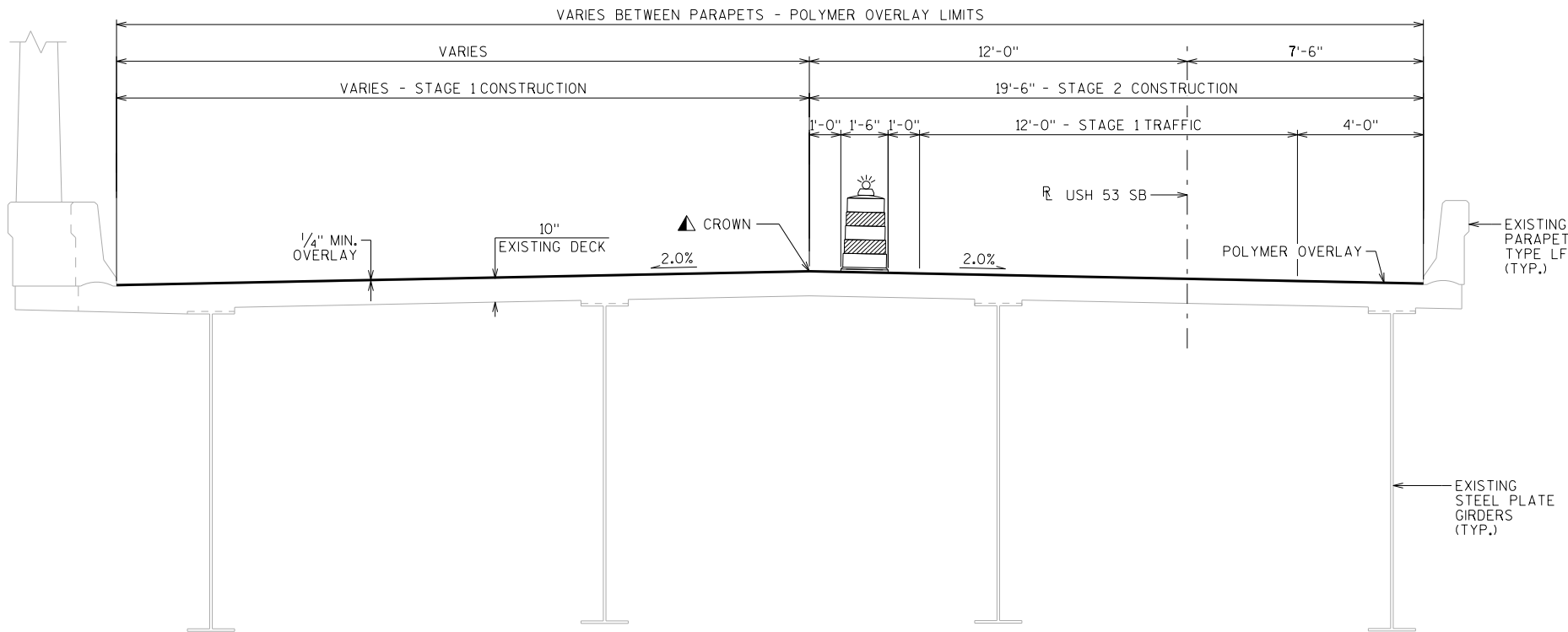
LIVE LOAD:  
INVENTORY RATING: HS-26  
OPERATING RATING: HS-44  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR - f'c = 4,000 P.S.I.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".  
AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.  
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".

LONGITUDINAL CONSTRUCTION JOINT




CROSS SECTION THRU ROADWAY

LOOKING UPSTATION  
(STAGE 1 TRAFFIC SHOWN, STAGE 2 IS SIMILAR)

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	4,600
SPV.0035	RAPID SET DECK REPAIR	CY	1

STRUCTURE DESIGN CONTACTS:  
MICHAEL LARSON (608) 267-4539  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
 <b>BUREAU OF STRUCTURES</b>			
ACCEPTED <i>William C. Dreher</i> <b>8/3/18</b>		CHIEF STRUCTURES DESIGN ENGINEER DATE	
<b>STRUCTURE B-18-168</b>			
USH 53 SB OVER EAU CLAIRE RIVER			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	ALTOONA
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	MJL	DESIGNED CK'D.	DLM
DRAWN BY	MJL	PLANS CK'D.	DLM
<b>POLYMER OVERLAY</b>			SHEET 1 OF 1

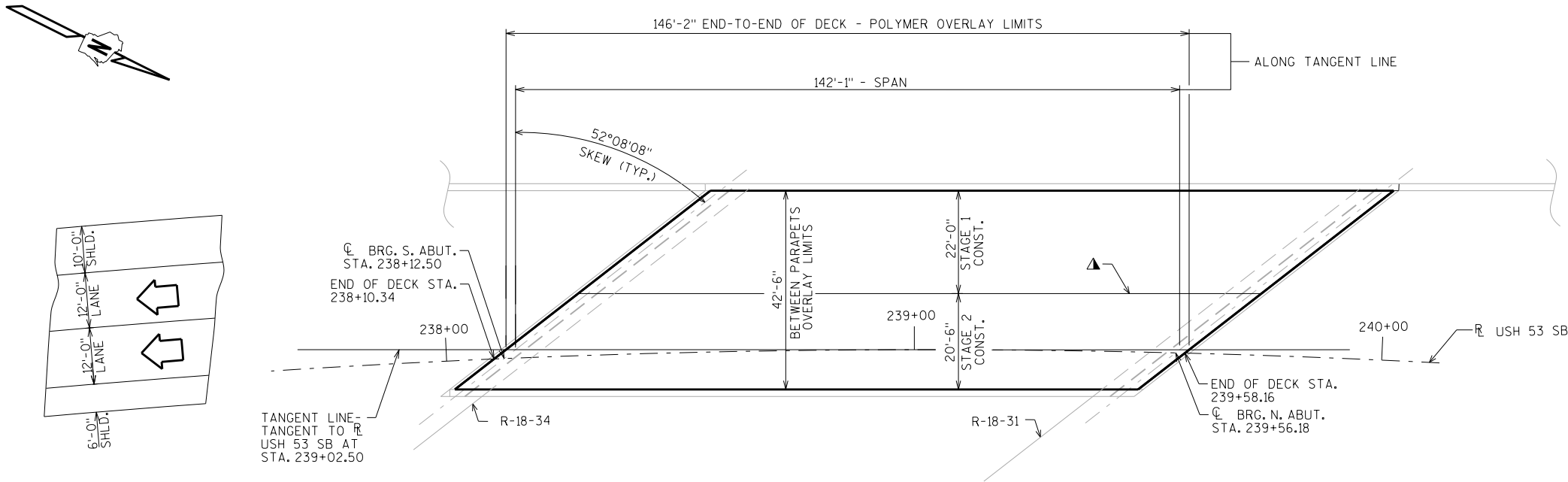
DESIGN DATA

LIVE LOAD:  
INVENTORY RATING; HS-26  
OPERATING RATING; HS-71  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR f'c = 4,000 P.S.I.

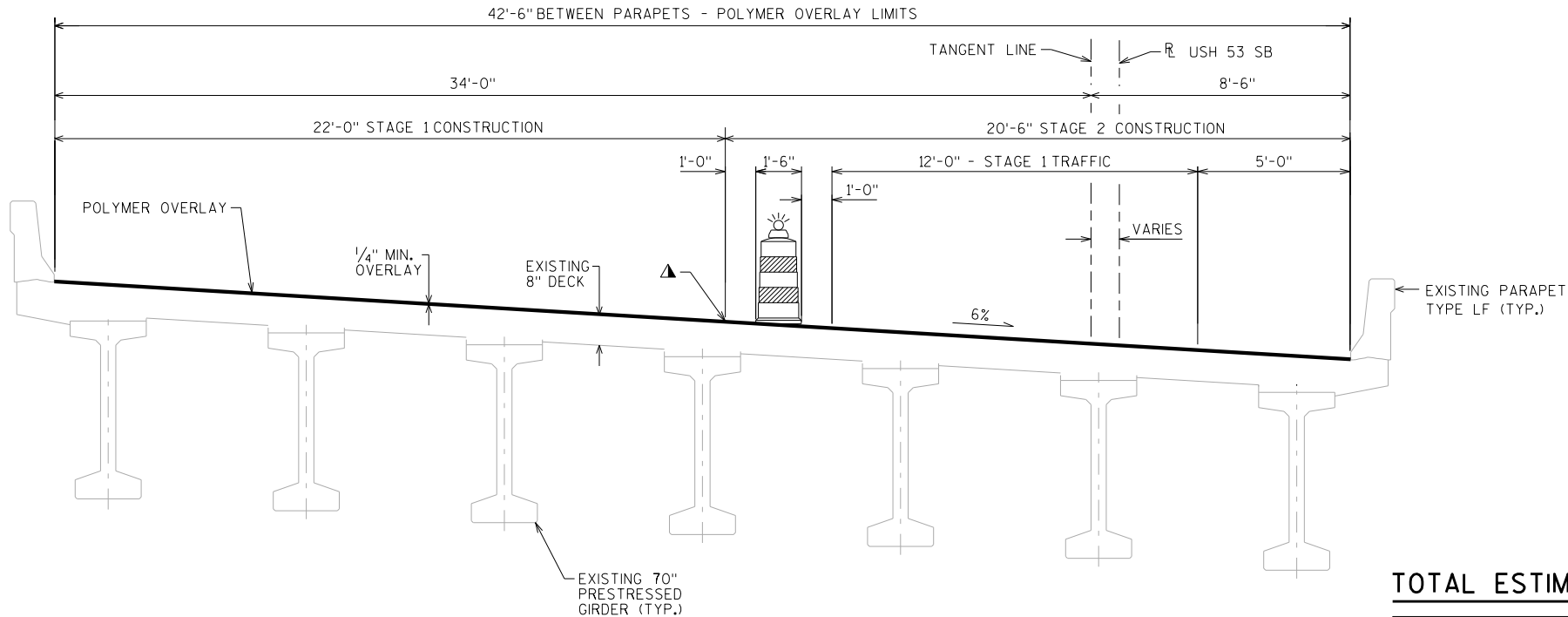
GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".  
AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.  
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".



PLAN

SINGLE SPAN 70" PRESTRESSED GIRDER BRIDGE



CURVE DATA

R USH 53 SB  
P.I. = STA. 234+24.11  
Δ = 36°06'38.5"  
D = 2°40'16.1"  
T = 699.24'  
L = 1351.89'  
R = 2145.00'  
S.E. = .06%  
P.C. = STA. 227+24.87  
P.T. = STA. 240+76.76

LONGITUDINAL CONSTRUCTION JOINT

LIST OF DRAWINGS

1. POLYMER OVERLAY


TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	691
SPV.0035	RAPID SET DECK REPAIR	CY	1

CROSS SECTION THRU ROADWAY

(STAGE 1 TRAFFIC SHOWN, STAGE 2 IS SIMILAR)  
(LOOKING UPSTATION)

STRUCTURE DESIGN CONTACTS:  
DANIEL MONROE (608) 266-8490  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
 <b>BUREAU OF STRUCTURES</b>			
ACCEPTED <i>William C. Dieker</i> 8/3/18		CHIEF STRUCTURES DESIGN ENGINEER DATE	
<b>STRUCTURE B-18-178</b>			
USH 53 SB OVER HASTINGS WAY			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	EAU CLAIRE
DESIGN SPEC. REHABILITATION - N/A			
DESIGNED BY	DLM	DESIGNED CK'D.	MJL
DRAWN BY	DLM	PLANS CK'D.	MJL
<b>POLYMER OVERLAY</b>			SHEET 1 OF 1

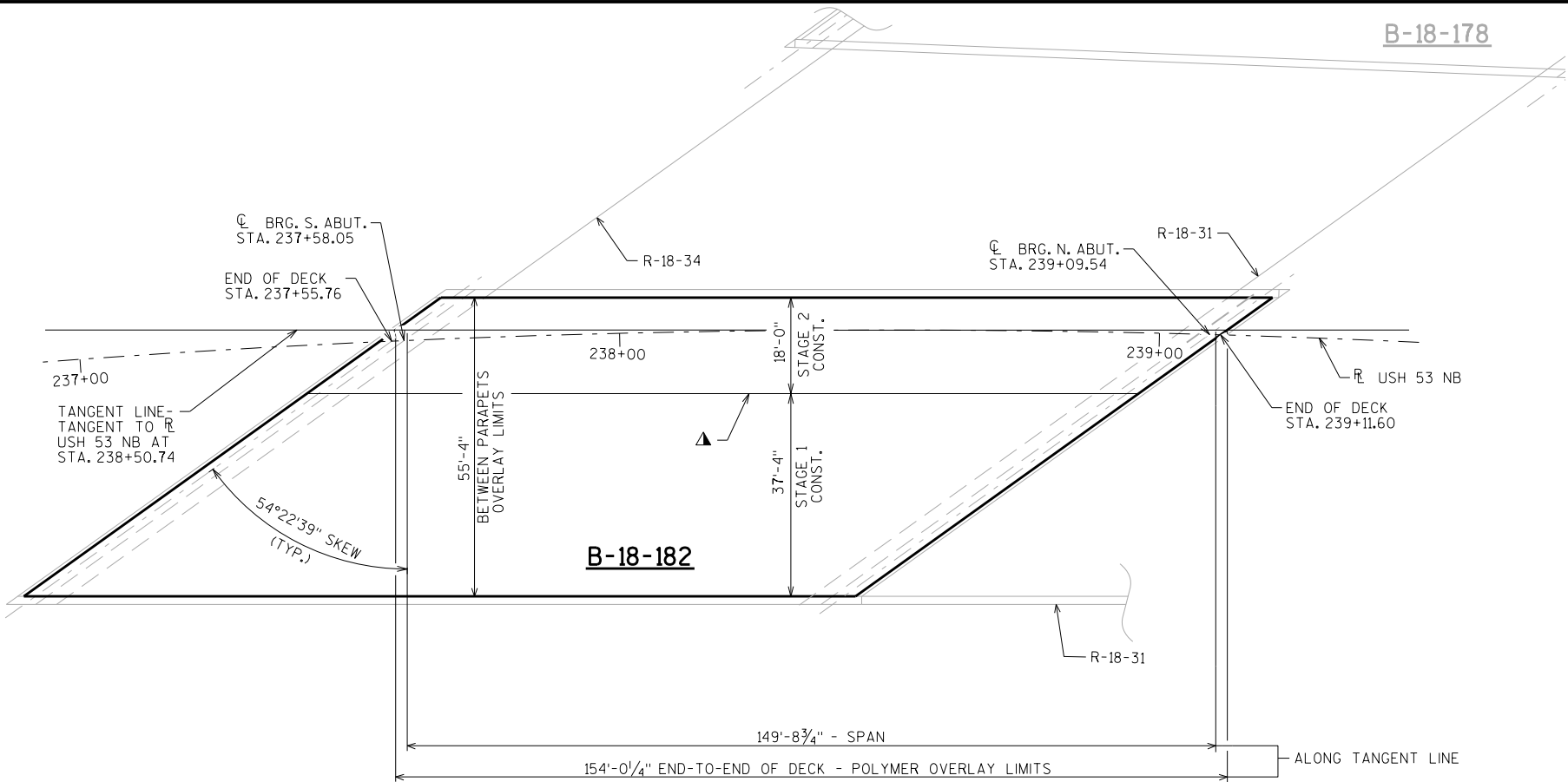
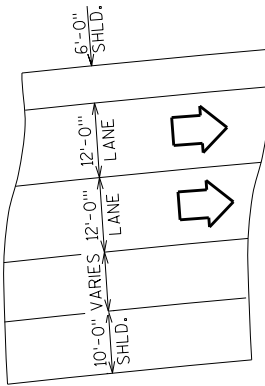
DESIGN DATA

LIVE LOAD:  
INVENTORY RATING; HS-25  
OPERATING RATING; HS-69  
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR f'c = 4,000 P.S.I.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".  
AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.  
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".



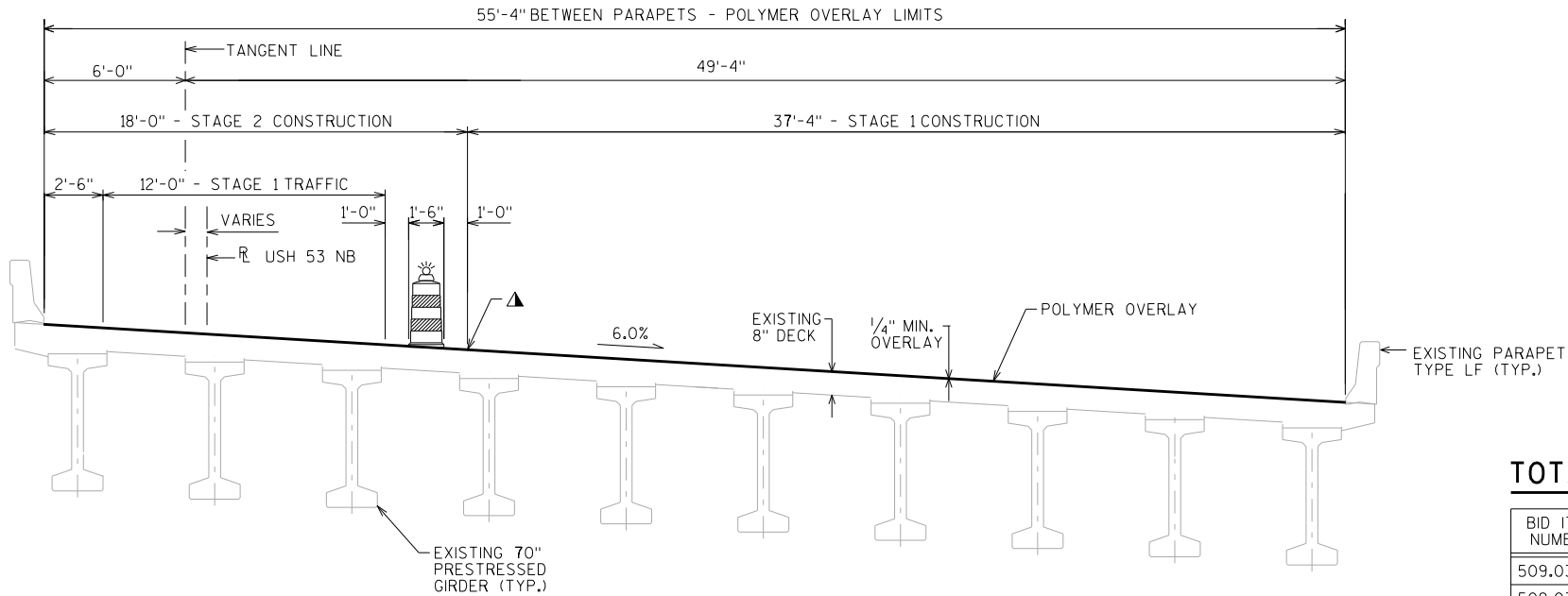
PLAN

SINGLE SPAN 70" PRESTRESSED GIRDER BRIDGE

CURVE DATA

R/ USH 53 NB  
P.I. = STA. 234+67.33  
Δ = 36°06'38.5"  
D = 2°44'52.8"  
T = 679.68'  
L = 1314.07'  
R = 2085.00'  
S.E. = .06%  
P.C. = STA. 227+87.65  
P.T. = STA. 241+01.72

▲ LONGITUDINAL CONSTRUCTION JOINT



CROSS SECTION THRU ROADWAY

(STAGE 1 TRAFFIC SHOWN, STAGE 2 IS SIMILAR)  
(LOOKING UPSTATION)

LIST OF DRAWINGS


1. POLYMER OVERLAY

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	947
SPV.0035	RAPID SET DECK REPAIR	CY	1

STRUCTURE DESIGN CONTACTS:

DANIEL MONROE (608) 266-8490  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
 <b>BUREAU OF STRUCTURES</b>			
ACCEPTED <i>William C. Dreher</i> 8/3/18		CHIEF STRUCTURES DESIGN ENGINEER DATE	
<b>STRUCTURE B-18-182</b>			
USH 53 NB OVER HASTINGS WAY			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	EAU CLAIRE
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	DLM	DESIGNED CK'D. MJL	DRAWN BY DLM PLANS CK'D. MJL
<b>POLYMER OVERLAY</b>			SHEET 1 OF 1

DESIGN DATA

LIVE LOAD:  
INVENTORY RATING; HS-25  
OPERATING RATING; HS-55  
MAXIMUM STANDARD PERMIT VEHICLE LOAD; 250 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR f'c = 4,000 P.S.I.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.

"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".

OVERLAY ON THE SIDES AND TOP OF THE MEDIAN IS INCLUDED IN THE QUANTITY ITEM "POLYMER OVERLAY"

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	943
SPV.0035	RAPID SET DECK REPAIR	CY	1


▲ LONGITUDINAL CONSTRUCTION JOINT

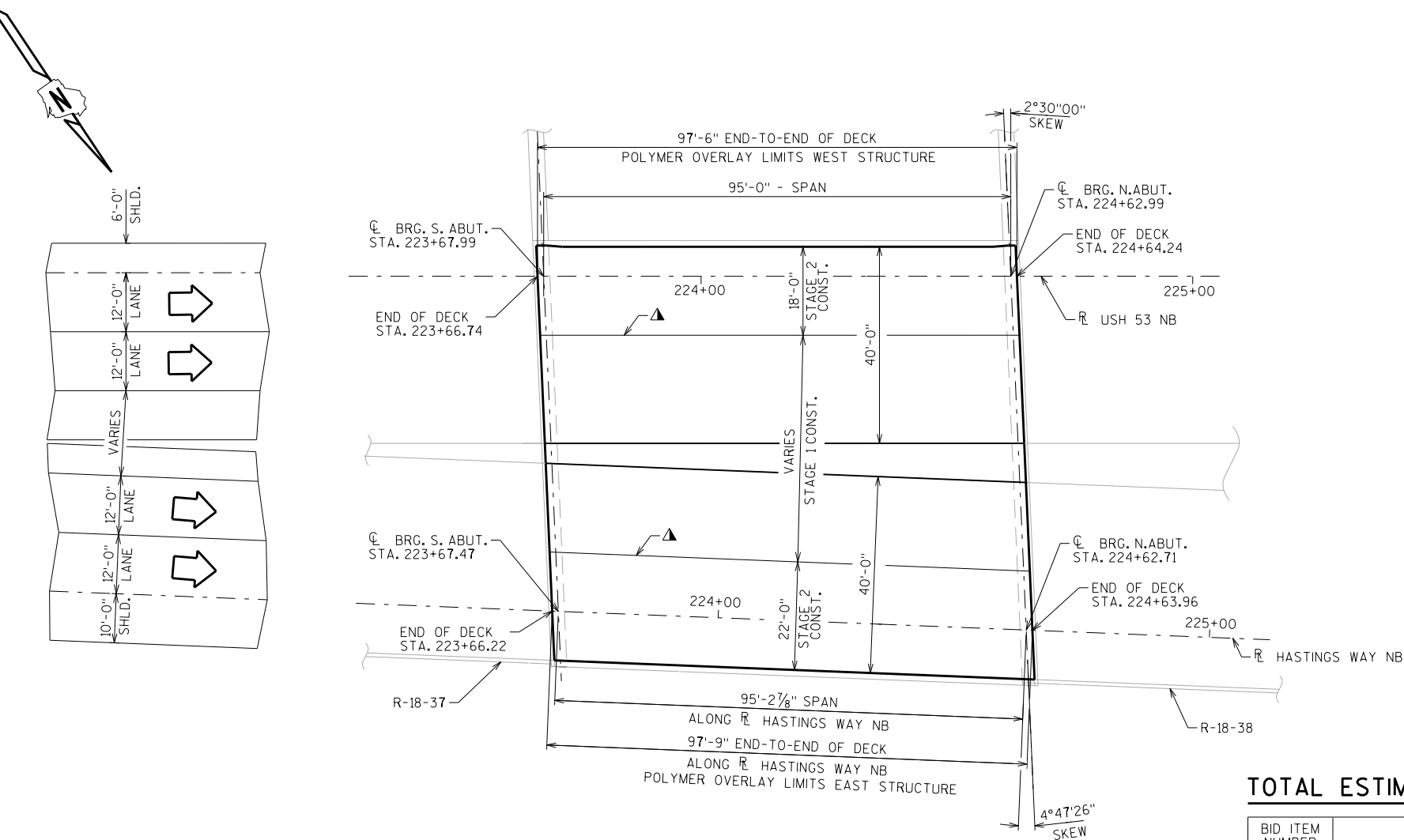
LIST OF DRAWINGS

1. POLYMER OVERLAY

STRUCTURE DESIGN CONTACTS:

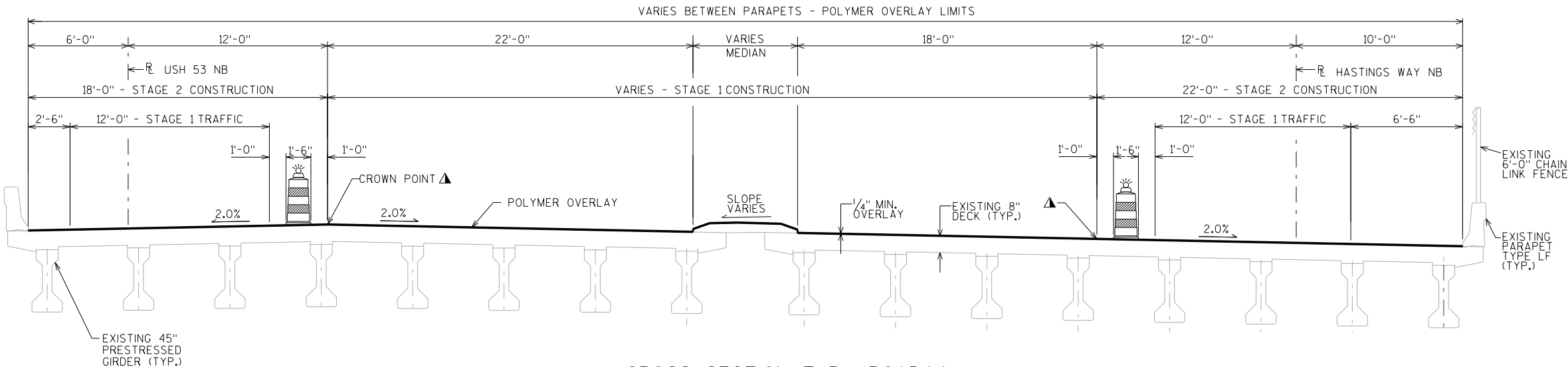
DANIEL MONROE (608) 266-8490  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
<div><div><b>BUREAU OF STRUCTURES</b></div><div>ACCEPTED <i>William C. Diehn</i> <b>8/3/18</b> CHIEF STRUCTURES DESIGN ENGINEER DATE</div></div>			
<b>STRUCTURE B-18-183</b>			
USH 53 NB OVER CYPRESS RD.			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	EAU CLAIRE
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	DESIGNED CK'D.	DRAWN BY	PLANS CK'D.
DLM	MJL	DLM	MJL
<b>POLYMER OVERLAY</b>			SHEET 1 OF 1



PLAN

SINGLE SPAN 45" PRESTRESSED GIRDER BRIDGE



CROSS SECTION THRU ROADWAY

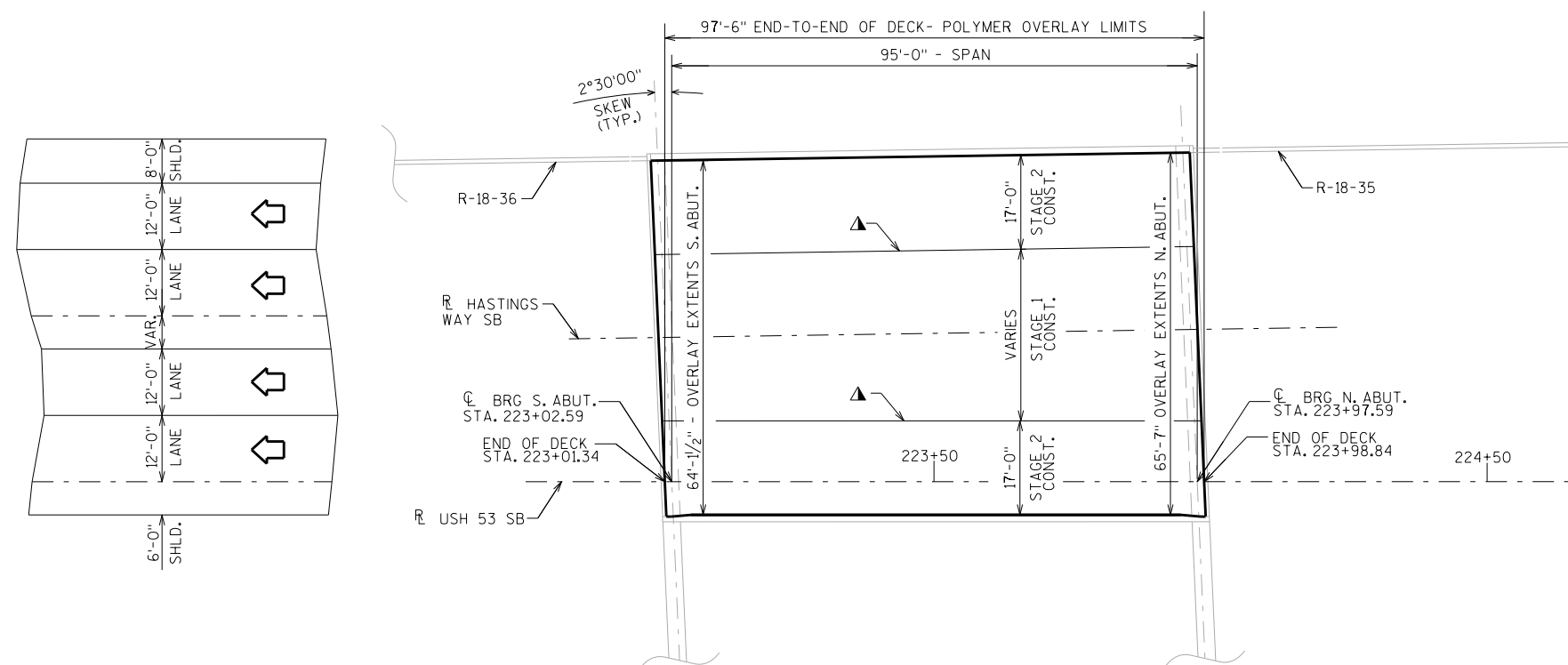
(STAGE 1 TRAFFIC SHOWN- STAGE 2 IS SIMILAR)  
(LOOKING UPSTATION)

RAPID SET DECK REPAIR —  $f'_c = 4,000$  P.S.I.

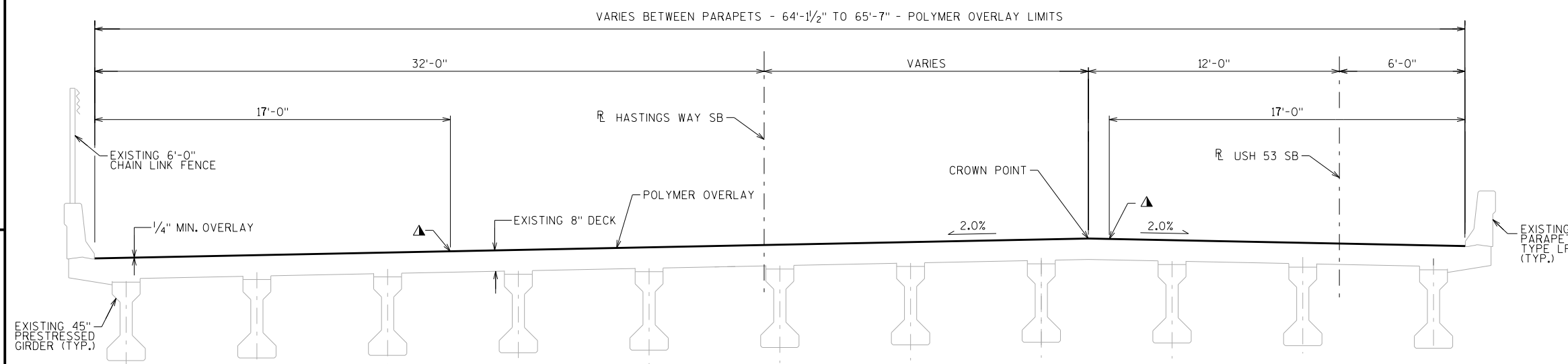
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR".

1. POLYMER OVERLAY
2. CONSTRUCTION STAGING

DANIEL MONROE	(608) 266-8490
LAURA SHADEWALD	(608) 267-9592

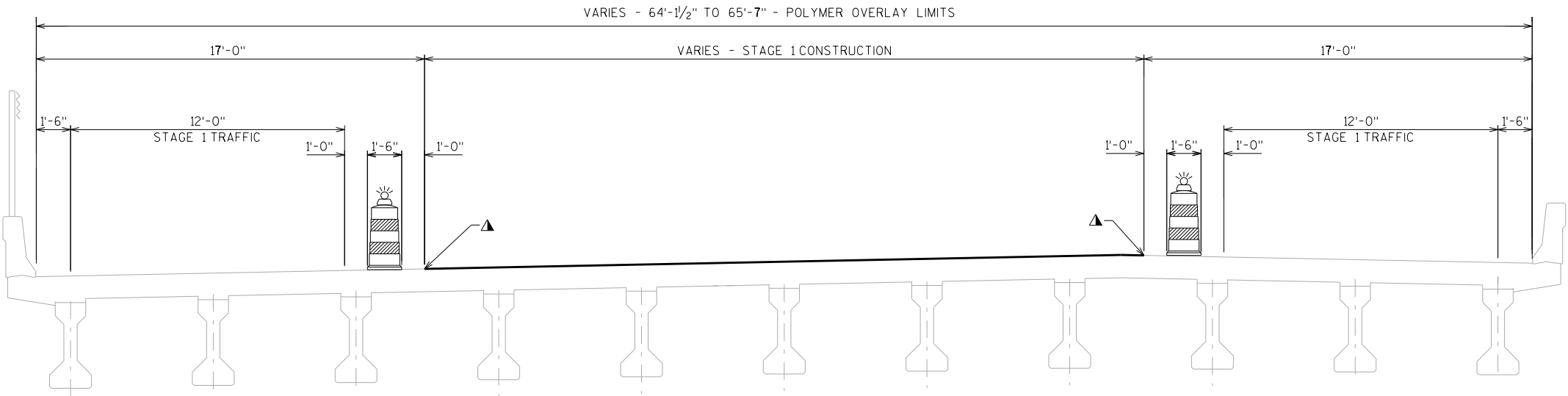
SCALE = 30.00

SINGLE SPAN 45" PRESTRESSED GIRDER BRIDGE

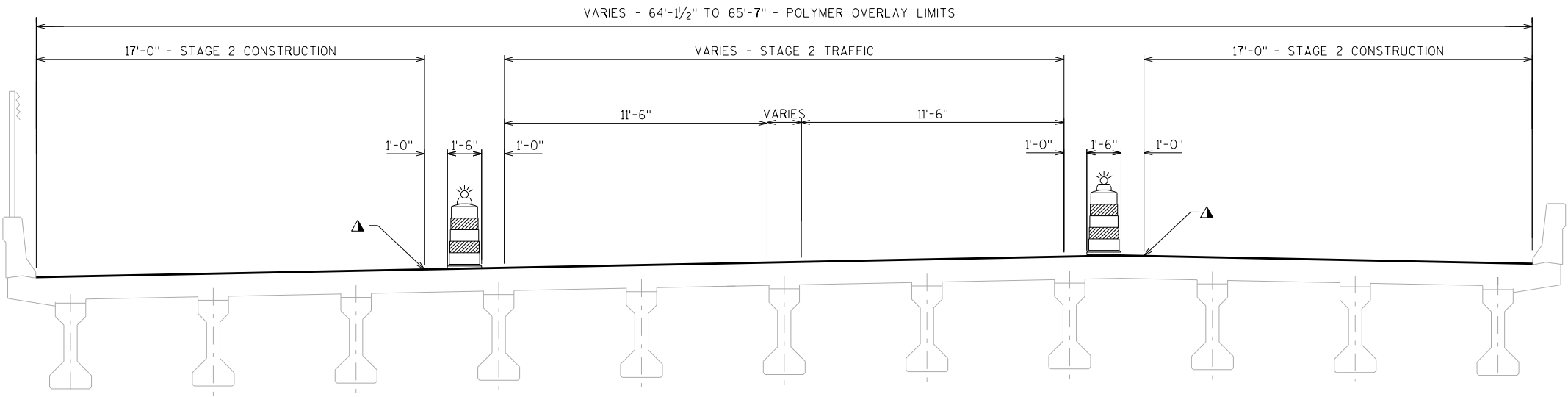


(LOOKING UPSTATION)  
(SEE SHT. 2 FOR CONST. STAGING)

BID ITEM NUMBER	BID ITEMS	UNIT	TOTAL
509.0301	PREPARATION DECKS TYPE 1	SY	8
509.0302	PREPARATION DECKS TYPE 2	SY	3
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	80
509.5100.S	POLYMER OVERLAY	SY	703
SPV.0035	RAPID SET DECK REPAIR	CY	1



**STAGE 1 CONSTRUCTION**  
(LOOKING UPSTATION)



**STAGE 2 CONSTRUCTION**  
(LOOKING UPSTATION)

▲ LONGITUDINAL CONSTRUCTION JOINT

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-18-184			
DRAWN BY		DLM	PLANS CK'D. <b>MJL</b>
CONSTRUCTION STAGING		SHEET 2	

SCALE = 30.00

DESIGN DATA

LIVE LOAD:  
INVENTORY RATING; HS-22  
OPERATING RATING; HS-44  
MAXIMUM STANDARD PERMIT VEHICLE LOAD; 250 KIPS

MATERIAL PROPERTIES:  
RAPID SET DECK REPAIR \_\_\_\_\_ f'c = 4,000 P.S.I.


GENERAL NOTES

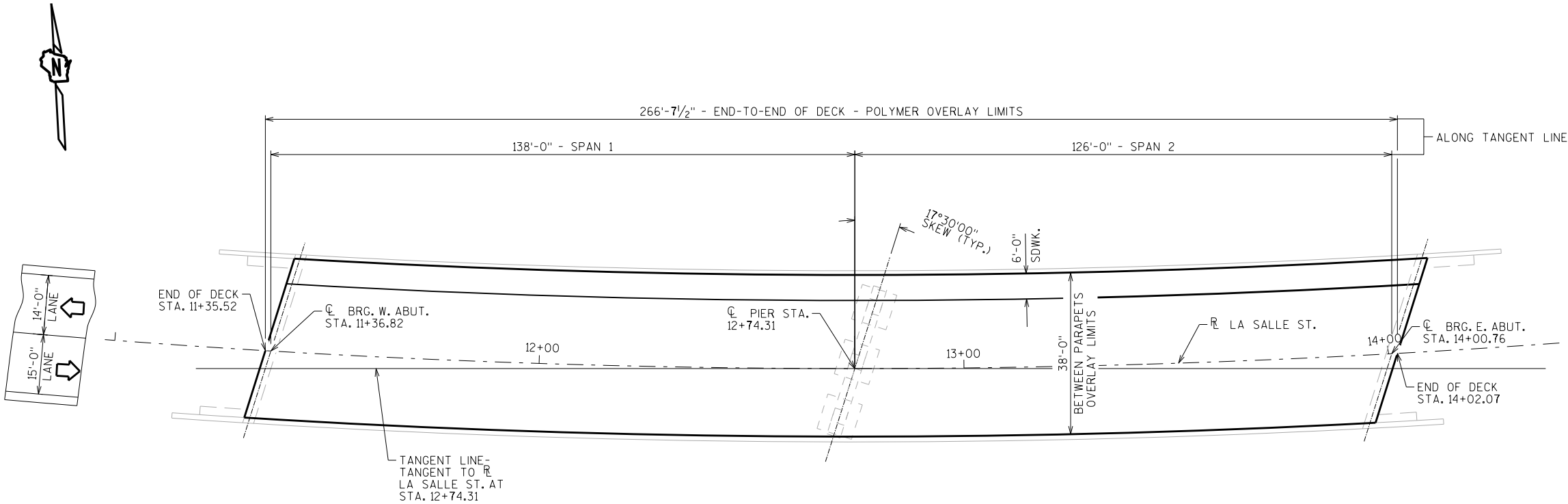
DRAWINGS SHALL NOT BE SCALED.  
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.  
DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".  
AREAS OF "PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" SHALL BE DEFINED BY A SAW CUT.  
"PREPARATION DECKS TYPE 1" AND "PREPARATION DECKS TYPE 2" AREAS ARE DETERMINED BY THE FIELD ENGINEER. DECK REPAIRS SHALL BE FILLED WITH "RAPID SET DECK REPAIR"  
OVERLAY ON THE SIDES AND TOP OF THE SIDEWALK IS INCLUDED IN THE QUANTITY FOR BID ITEM "POLYMER OVERLAY".

CURVE DATA

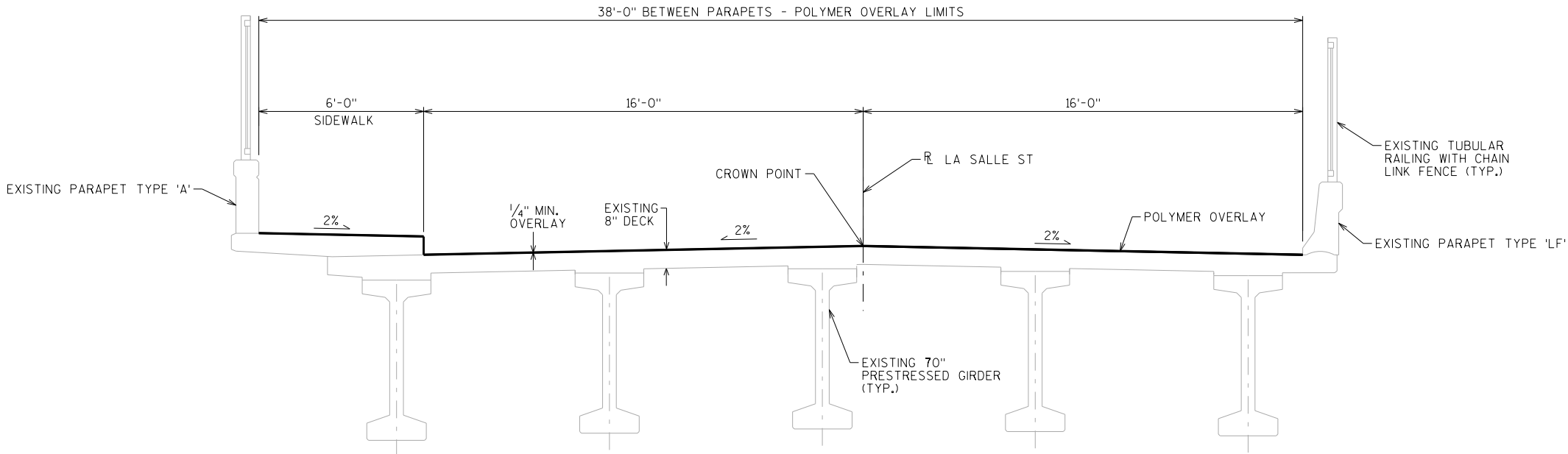
LA SALLE STREET  
P.I. STA. = 14+87.09  
Δ = 08°89'48"  
R = 5729.58'  
T = 408.86'  
L = 816.34'  
E = 14.57'

STRUCTURE DESIGN CONTACTS:  
DANIEL MONROE (608) 266-8490  
LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
<div><div><div><div><div>BUREAU OF</div><div>STRUCTURES</div></div><div><div>ACCEPTED</div><div>CHIEF STRUCTURES DESIGN ENGINEER</div><div>DATE</div></div></div><div><div>William C. Dreher</div><div>8/3/18</div></div></div></div>			
STRUCTURE B-18-190			
LA SALLE ST. OVER USH 53			
COUNTY	EAU CLAIRE	TOWN/CITY/VILLAGE	SEYMOUR
DESIGN SPEC.	REHABILITATION N/A		
DESIGNED BY	DLM	DESIGNED CK'D.	MJL
DRAWN BY	DLM	PLANS CK'D.	MJL
POLYMER OVERLAY			SHEET 1 OF 1



PLAN  
2 SPAN 70" PRESTRESSED GIRDER BRIDGE



CROSS SECTION THRU ROADWAY  
(LOOKING UPSTATION)

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	2
509.0302	PREPARATION DECKS TYPE 2	SY	1
509.0310.S	SAWING PAVEMENT DECK PREPARATION AREAS	LF	20
509.5100.S	POLYMER OVERLAY	SY	1146
SPV.0035	RAPID SET DECK REPAIR	CY	1

LIST OF DRAWINGS

1. POLYMER OVERLAY



## ***Wisconsin Department of Transportation***

Dedicated people creating transportation solutions  
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>