

WIS

PROJECT ID:
WITH: N/A

6999-03-70

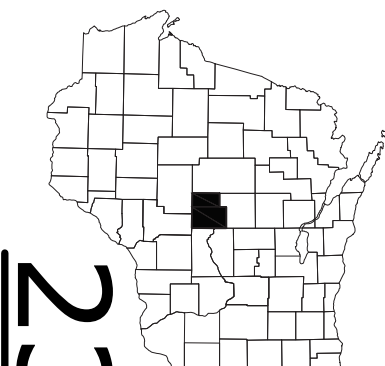
COUNTY: WOOD

DECEMBER 2018

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 142



DESIGN DESIGNATION

A.A.D.T. 2018	=	9,250
A.A.D.T. 2038	=	10,150
D.H.V.	=	1,390
D.D.	=	59/41
T.	=	9.8%
DESIGN SPEED	=	35 MPH
ESALS	=	2,600,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

ROCK	
LABEL	
95.36	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

C WISCONSIN RAPIDS, SECOND AVE SO

GAYNOR AVENUE TO STH 13

STH 54
WOOD COUNTY

STATE PROJECT NUMBER
6999-03-70



TOTAL NET LENGTH OF CENTERLINE = 0.177 MI

COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), WOOD COUNTY NAD 83, (2011) ADJUSTMENT.
ALL ELEVATIONS ON THIS PROJECT ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), (2012) ADJUSTMENT.

STATE PROJECT

6999-03-70

FEDERAL PROJECT

PROJECT

CONTRACT

ACCEPTED FOR

CITY of WISCONSIN RAPIDS

10/27/17
(Date)

Joe Eichholtz
City Engineer

(Signature & Title of Official)

emcs inc

500 North 17th Avenue
Wausau, WI 54401
715.845.1081 Fax 715.845.1099



10-20-2017
(Date)

Stephanie G. Christensen
(Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	EMCS, INC.
Designer	EMCS, INC.
Project Manager	WENDY ARNESON
Regional Examiner	CHERYL SIMON
Regional Supervisor	MIKE KRETSCHMER

APPROVED FOR THE DEPARTMENT

DATE: 10/30/2017

Wendy Arneson
(Signature)

E

GENERAL NOTES

NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

AS-BUILTS USED FOR PLAN DEVELOPMENT

PROJECT NO: U6903(1), CONSTRUCTION YEAR: 1954
PROJECT NO: T6903, CONSTRUCTION YEAR: 1955
PROJECT NO: 6999-06-06, CONSTRUCTION YEAR: 1979
PROJECT NO: 6999-06-05, CONSTRUCTION YEAR: 1980
PROJECT NO: 6999-05-61, CONSTRUCTION YEAR: 2000
PROJECT NO: 6390-00-72, CONSTRUCTION YEAR: 2004

ORDER OF SECTION 2 SHEETS

- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PAVING DETAILS
- EROSION CONTROL
- PERMANENT SIGNING & PAVEMENT MARKING
- LIGHTING
- TRAFFIC CONTROL

UTILITIES

CHARTER COMMUNICATIONS
(COMMUNICATIONS)
JESSE GRUNNY
503 EAST IVES STREET, #316
MARSHFIELD, WI 54449
(715) 651-5605
JESSE.GRUNY@CHARTER.COM

CITY OF WISCONSIN RAPIDS
(TRAFFIC SIGNALS)
JOE EICHSTEADT
444 WEST GRAND AVE
WISCONSIN RAPIDS, WI 54495
(715) 421-8251
JEICHSTEADT@WIRAPIDS.ORG

CITY OF WISCONSIN RAPIDS
(SEWER)
JOE EICHSTEADT
444 WEST GRAND AVE
WISCONSIN RAPIDS, WI 54495
(715) 421-8251
JEICHSTEADT@WIRAPIDS.ORG

CITY OF WISCONSIN RAPIDS
WATER WORKS AND LIGHT
(WATER)
TODD WEILER
221 16TH STREET SOUTH
WISCONSIN RAPIDS, WI 54495
(715) 422-9013
TODD.WEILER@WRWWLC.COM

CITY OF WISCONSIN RAPIDS
WATER WORKS AND LIGHT
(WATER & ELECTRIC)
TODD WEILER
221 16TH STREET SOUTH
WISCONSIN RAPIDS, WI 54495
(715) 422-9013
TODD.WEILER@WRWWLC.COM

SOLARUS
(COMMUNICATIONS)
RICK SCHMUTZER
440 EAST GRAND AVE
WISCONSIN RAPIDS, WI 54494
(715) 421-8193
SCHMUTZ@SOLARUS.NET

WE ENERGIES
(GAS/PETROLEUM)

SEND ALL WE ENERGIES
CORRESPONDENCE TO:

NICOLE SMULLEN
333 W EVERETTE STREET,
A299
MILWAUKEE, WI 53203
(414) 221-5617
NICOLE.SMULLEN@WECENERGYGROUP.COM

WE ENERGIES PROJECT
CONSTRUCTION FIELD CONTACT:

RYAN MIENTKE
1921 8TH STREET SOUTH
WISCONSIN RAPIDS, WI 54494
OFFICE: (715) 421-7249
CELL: (715) 421-9293
RYAN.MIENTKE@WE-ENERGIES.COM

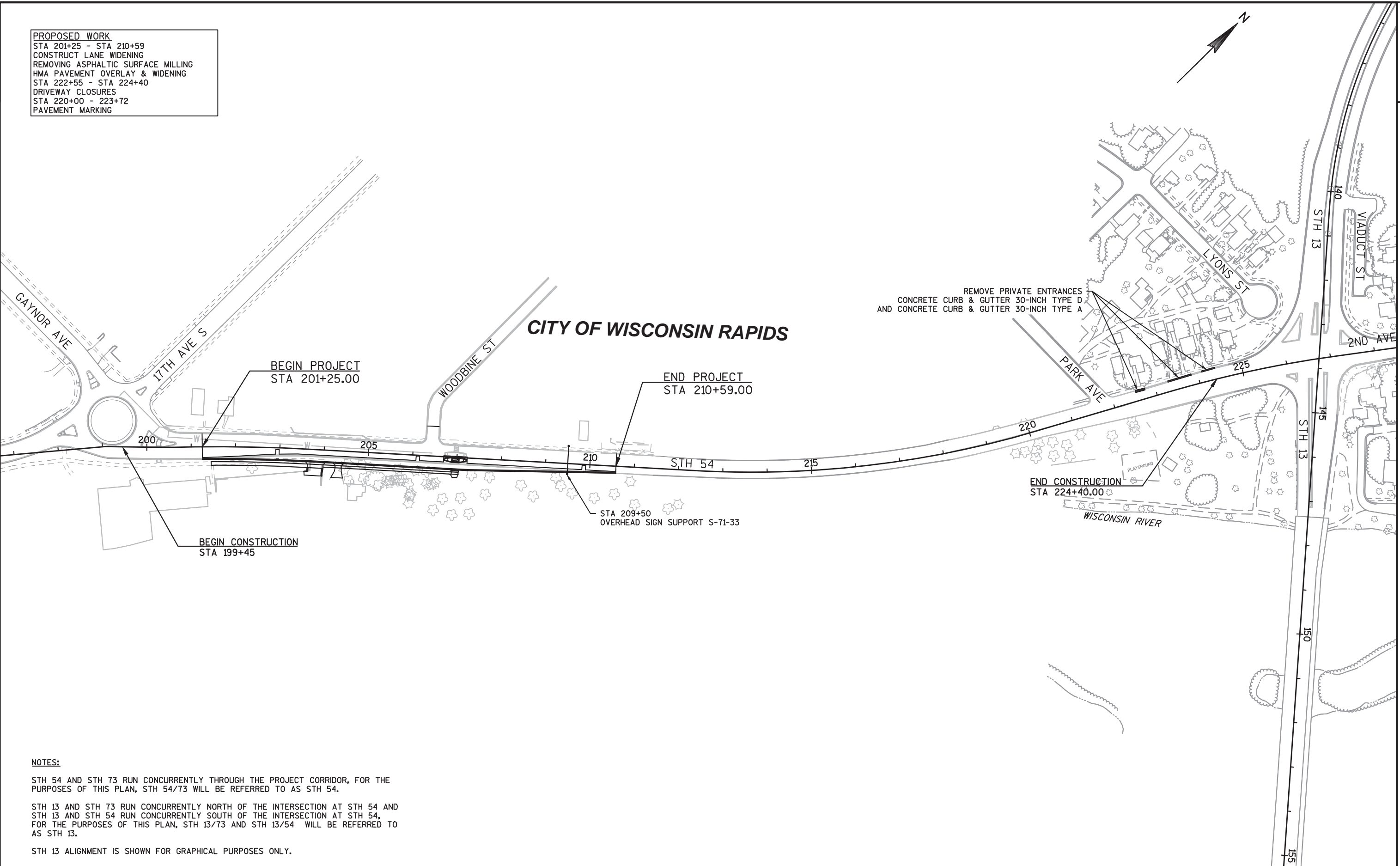


Dial **811** or (800)242-8511
www.DiggersHotline.com

OTHER CONTACTS

DNR LIAISON
BRAD BETTHAUSER
473 GRIFFITH DRIVE
WISCONSIN RAPIDS, WI 54494
(715) 421-7851
BRAD.BETTHAESER@WISCONSIN.GOV

PROPOSED WORK
STA 201+25 - STA 210+59
CONSTRUCT LANE WIDENING
REMOVING ASPHALTIC SURFACE MILLING
HMA PAVEMENT OVERLAY & WIDENING
STA 222+55 - STA 224+40
DRIVEWAY CLOSURES
STA 220+00 - 223+72
PAVEMENT MARKING



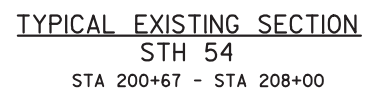
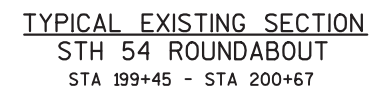
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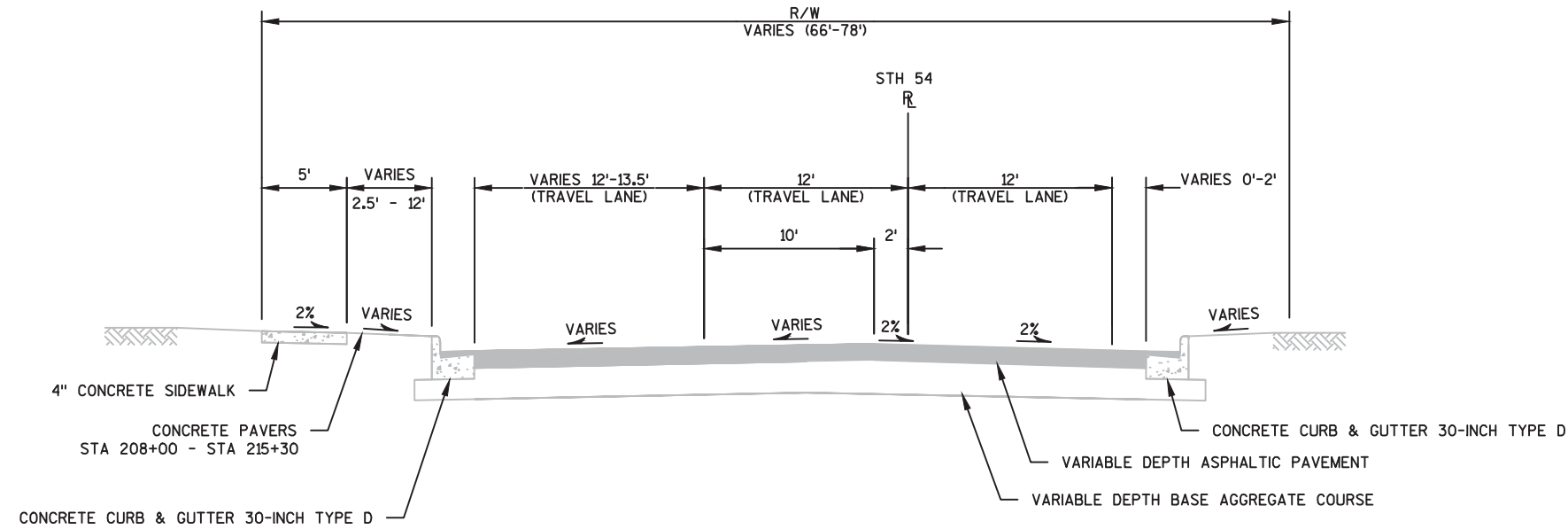
STH 54 AND STH 73 RUN CONCURRENTLY THROUGH THE PROJECT CORRIDOR, FOR THE PURPOSES OF THIS PLAN, STH 54/73 WILL BE REFERRED TO AS STH 54.

STH 13 AND STH 73 RUN CONCURRENTLY NORTH OF THE INTERSECTION AT STH 54 AND STH 13 AND STH 54 RUN CONCURRENTLY SOUTH OF THE INTERSECTION AT STH 54. FOR THE PURPOSES OF THIS PLAN, STH 13/73 AND STH 13/54 WILL BE REFERRED TO AS STH 13.

STH 13 ALIGNMENT IS SHOWN FOR GRAPHICAL PURPOSES ONLY.

PROJECT NO:6999-03-70	HWY:STH 54	COUNTY:WOOD	PROJECT OVERVIEW	SHEET	E
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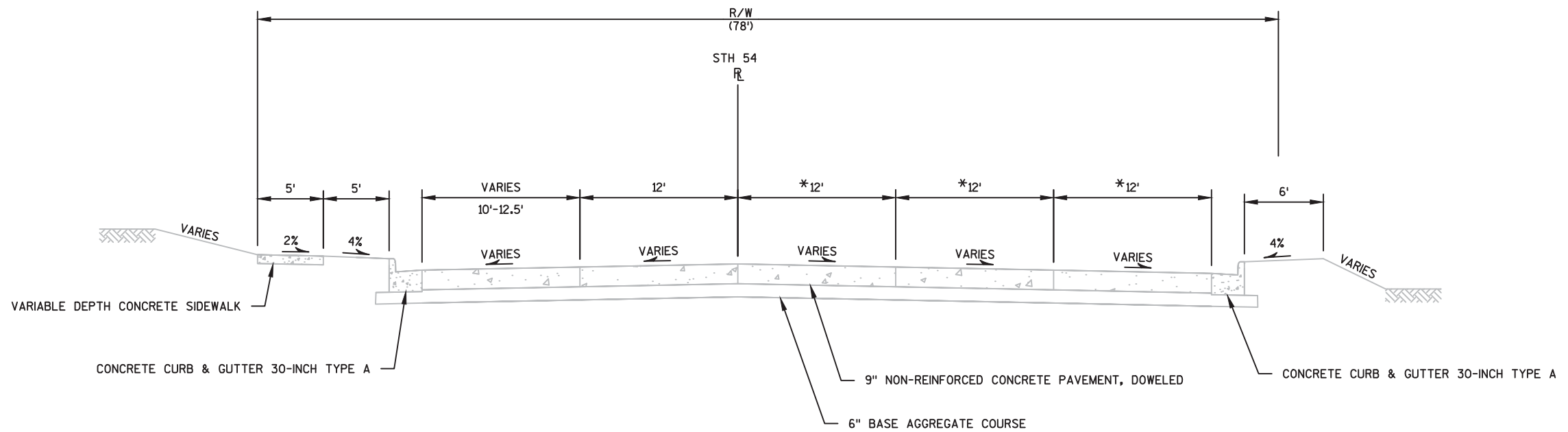




TYPICAL EXISTING SECTION

STH 54

STA 208+00 - STA 222+75



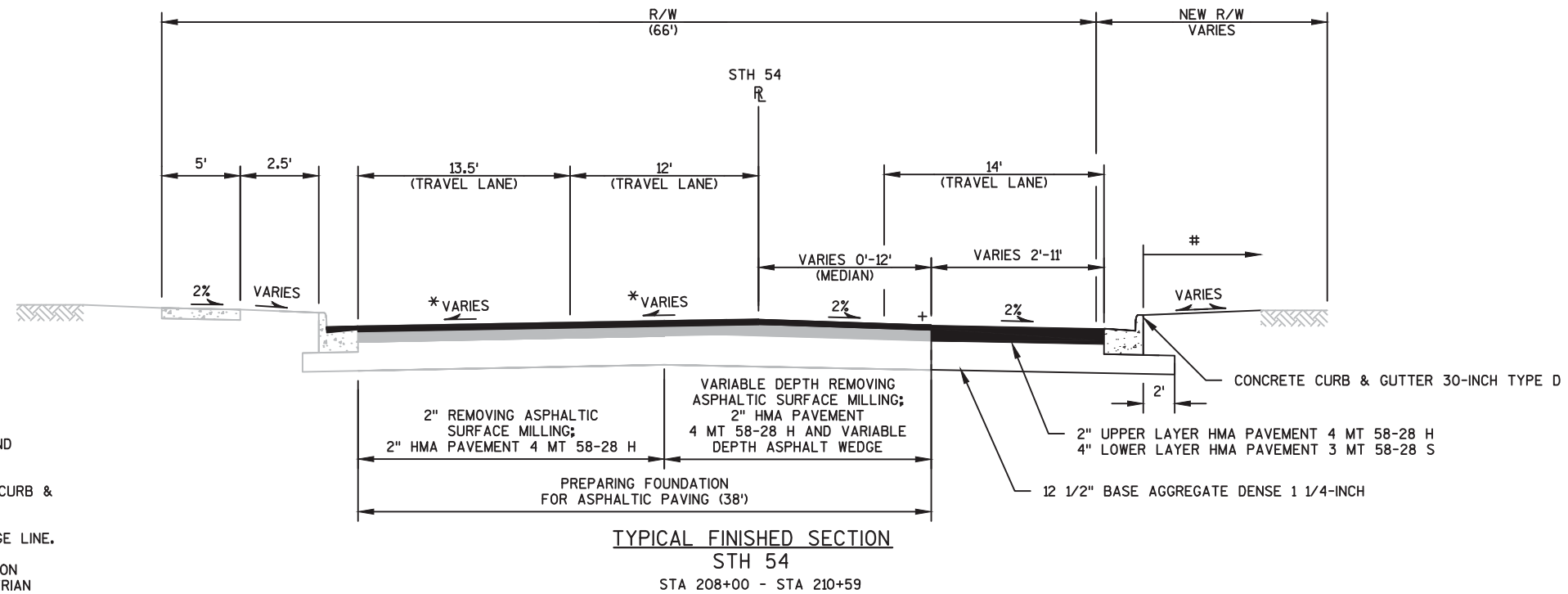
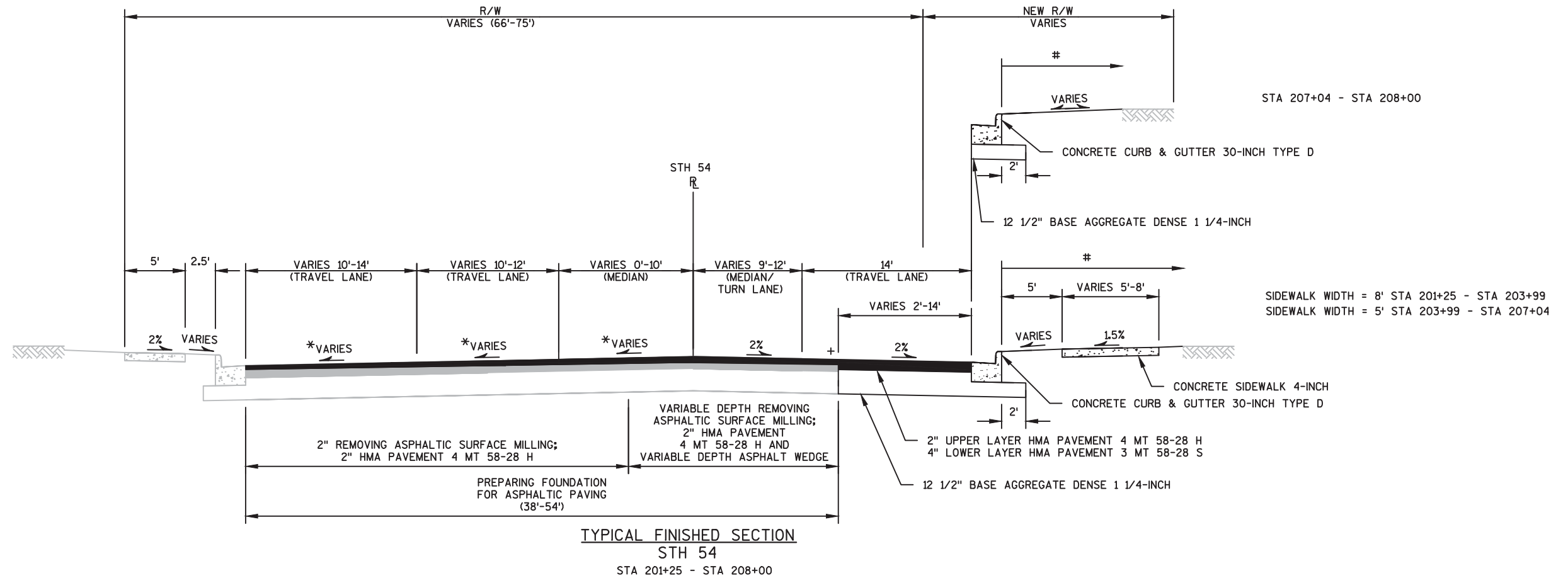
TYPICAL EXISTING SECTION

STH 54

STA 222+75 - STA 224+40

NOTE

* LANE WIDTHS VARY IN THE TURN LANE TRANSITION AREA.



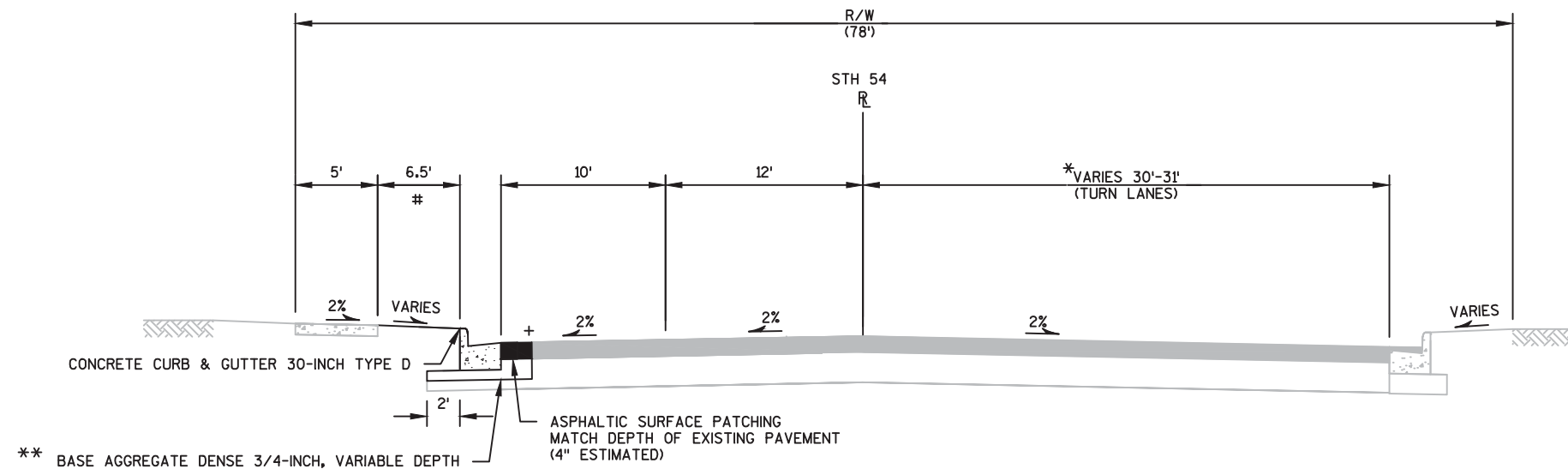
NOTES

TOPSOIL, SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS 1 TYPE A.

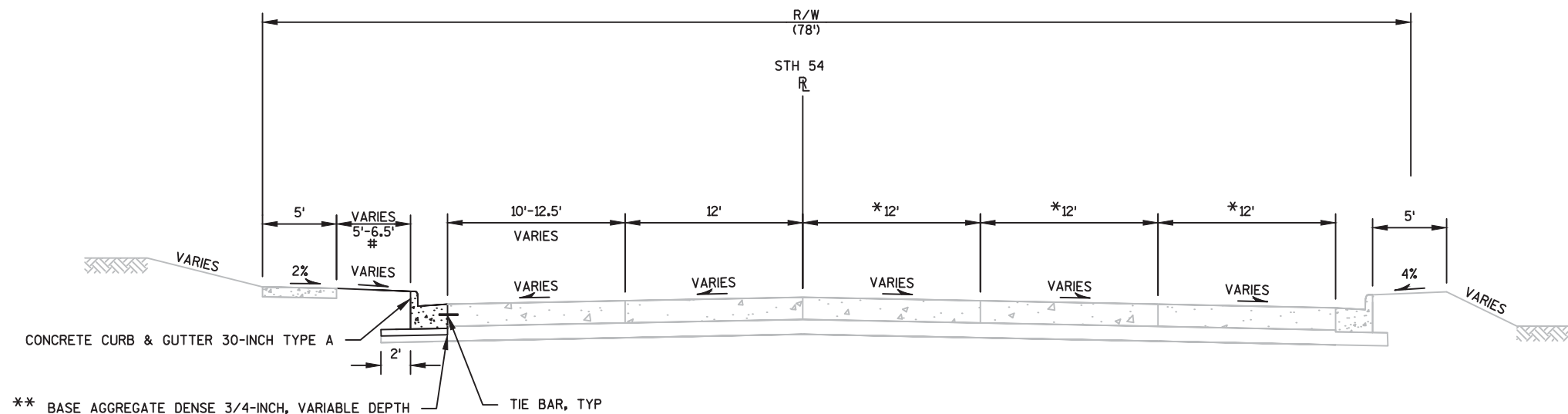
* CROSS SLOPE VARIES FROM 1.4% - 3.2%. MATCH EXISTING CURB & GUTTER FLANGE LINE ELEVATION.

+ SAWING ASPHALT 2' FROM EXISTING CURB & GUTTER FLANGE LINE.

SEE TRAFFIC CONTROL TYPICAL SECTIONS AND CONSTRUCTION DETAILS FOR ASPHALTIC PATCH AREA AROUND THE PEDESTRIAN REFUGE ISLAND.



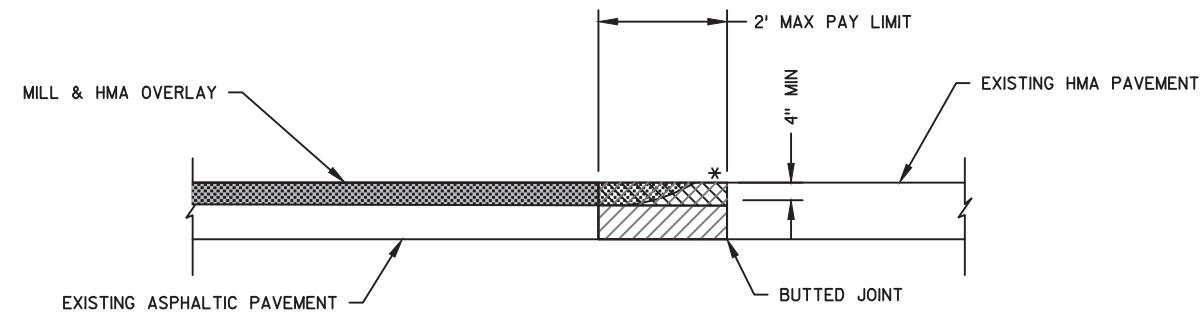
TYPICAL FINISHED SECTION
STH 54
STA 222+55 - STA 222+75






TYPICAL FINISHED SECTION
STH 54
STA 223+31 - STA 223+86
STA 224+10 - STA 224+40

NOTES

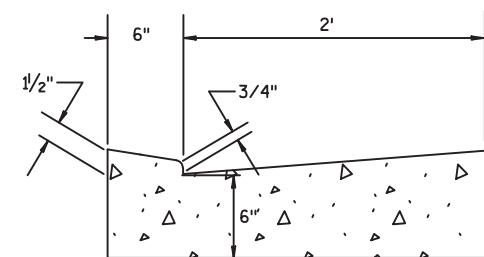
- * LANE WIDTHS VARY IN THE TURN LANE TRANSITION AREA
- ** PLACE BASE AGGREGATE DENSE 3/4-INCH UNDER THE PROPOSED CONCRETE CURB & GUTTER 30-INCH TYPE A AND TYPE D AS NEEDED
- # TOPSOIL, SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS 1 TYPE A
- + SAWING ASPHALT 2' FROM EXISTING CURB & GUTTER FLANGE LINE



-  ASPHALTIC WEDGING
(FULL DEPTH REMOVAL OPTIONAL)
-  REMOVING ASPHALTIC SURFACE BUTT JOINTS
DO NOT REMOVE MATERIAL UNDER THIS ITEM UNTIL 24 HOURS BEFORE PAVING
-  REMOVING ASPHALTIC SURFACE MILLING
- * SAWCUT REQD FOR FULL DEPTH REMOVAL OPTION

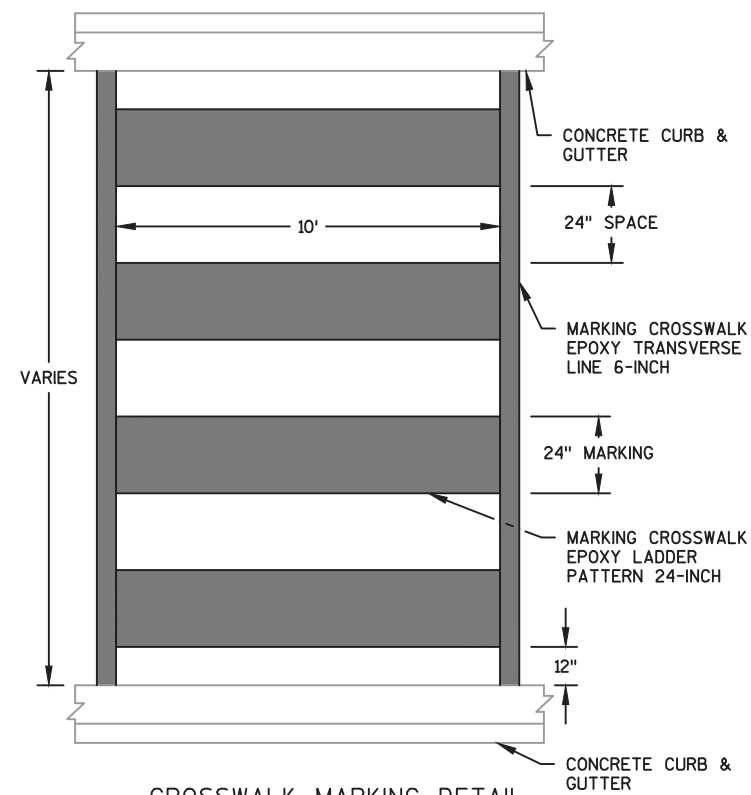
ASPHALTIC SURFACE BUTT JOINT DETAIL

BEGIN PROJECT (STA 201+25)
END PROJECT (STA 210+59)
WOODBINE ST



CONCRETE CURB & GUTTER 30-INCH WITH NO CURB HEAD OR FOR DRIVEWAYS

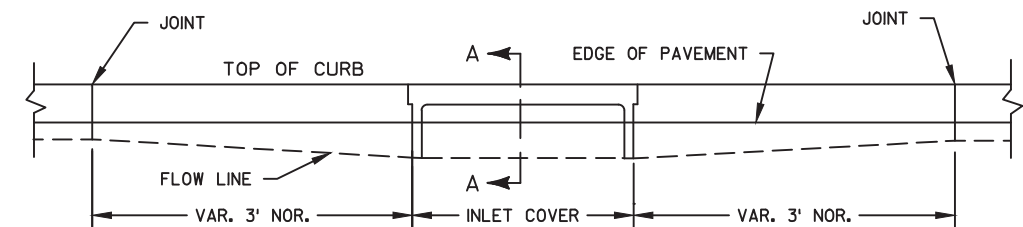
CONCRETE CURB & GUTTER FOR DRIVEWAYS
TO BE PAID FOR AS CONCRETE CURB & GUTTER ITEM
(FOR DETAILS NOT SHOWN, SEE "SDD CONCRETE CURB,
CONCRETE CURB & GUTTER AND TIES")



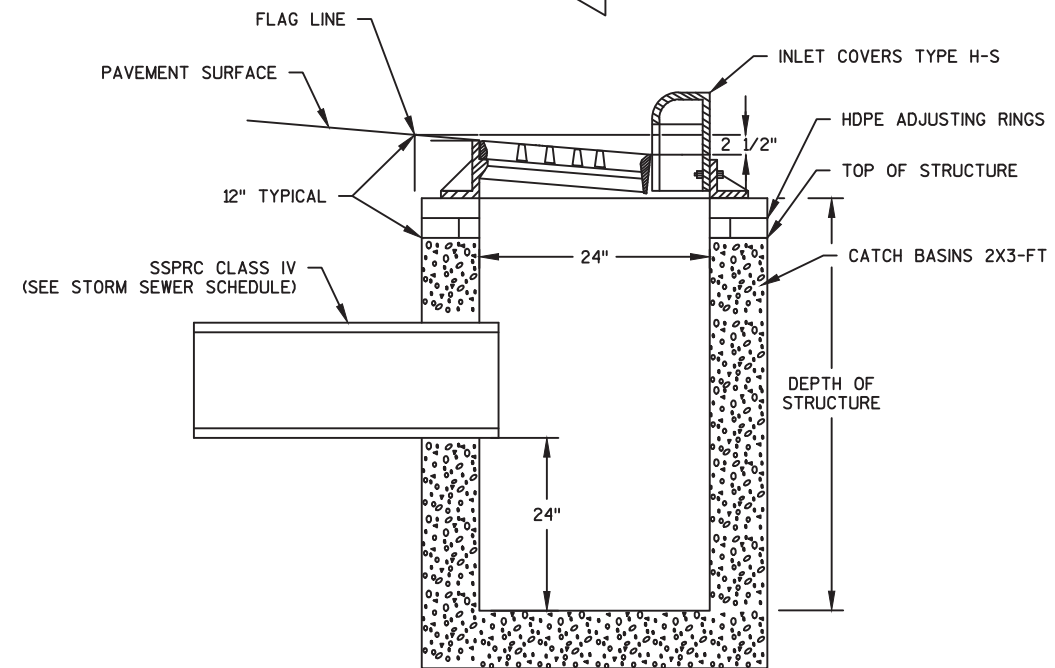
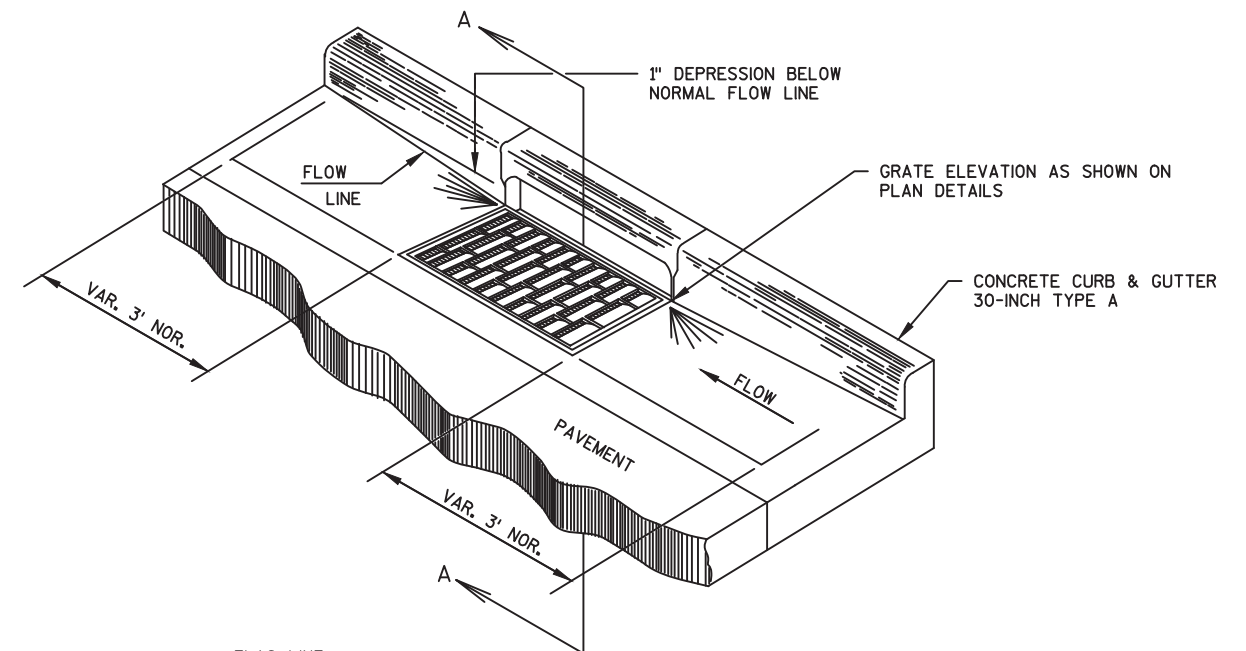
CROSSWALK MARKING DETAIL

NOTE
PLACE MARKING LINES
TO AVOID WHEEL PATHS

STA 200+25, RT
STA 200+38, LT
STA 206+96, LT & RT



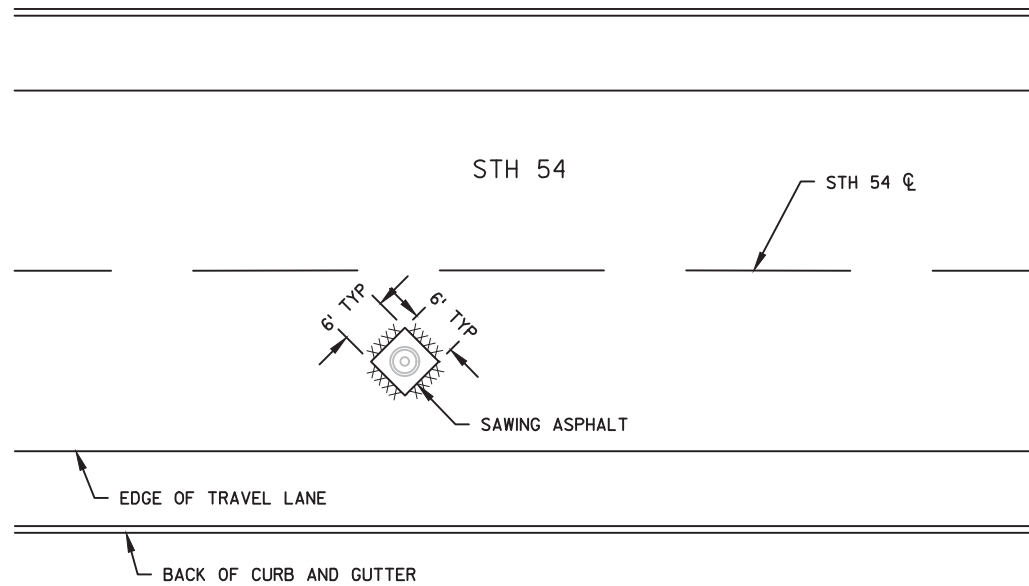
ELEVATION



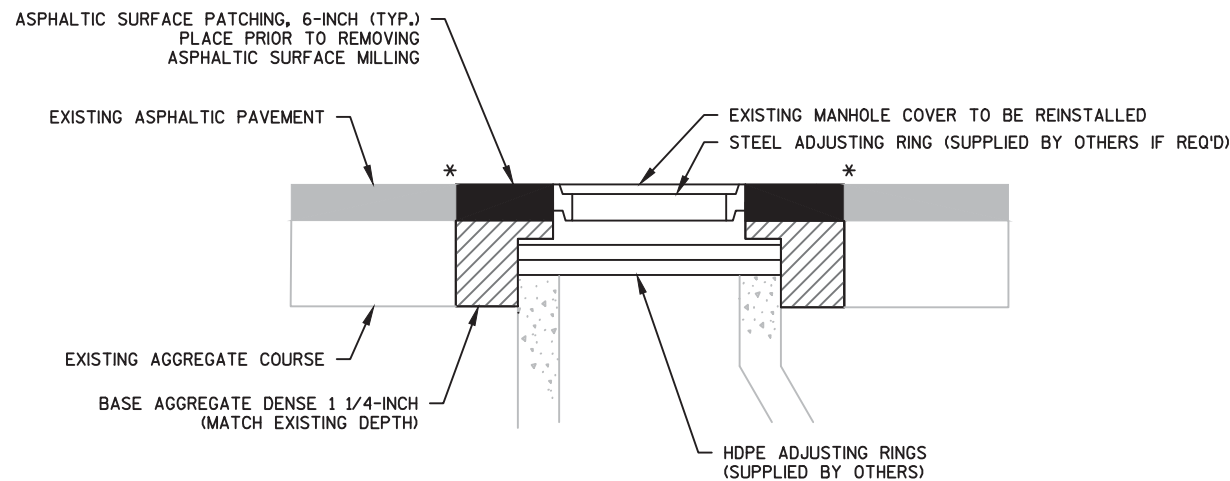
SECTION A-A

DETAIL OF CURB & GUTTER AT INLETS

(TYPE 2X3-FT/H-S INLET SHOWN)
STA 202+95.00, RT
STA 206+12.27, RT
STA 209+87.73, RT



PLAN VIEW



ADJUSTING MANHOLE COVERS AND SANITARY SEWER MANHOLE COVERS

STA 201+25 - STA 210+59

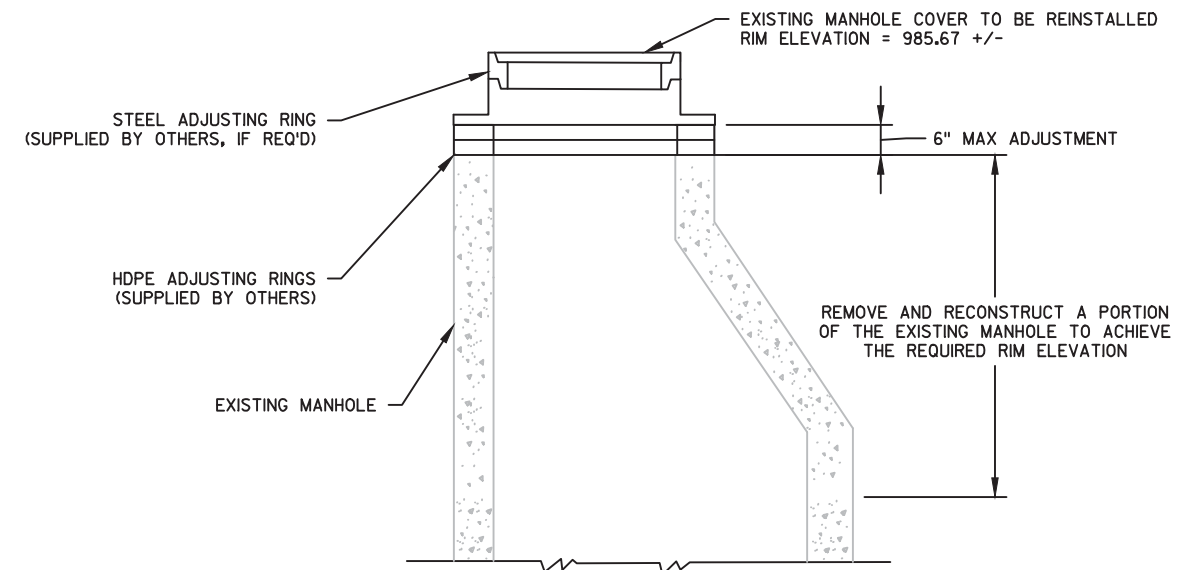
LEGEND

* SAWCUT REQD

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 1.82 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.7 ACRES

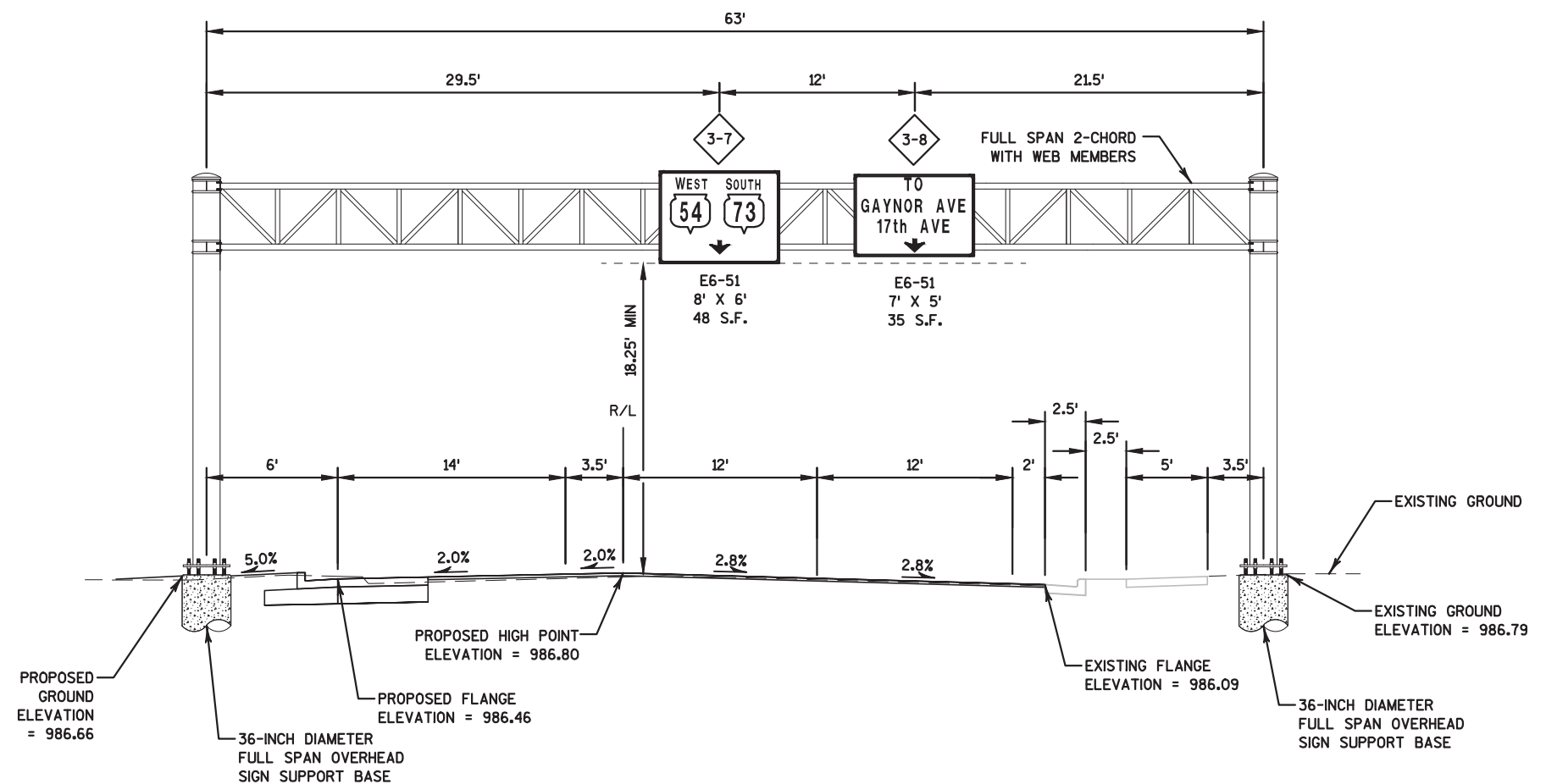


RECONSTRUCTING SANITARY MANHOLE DETAIL

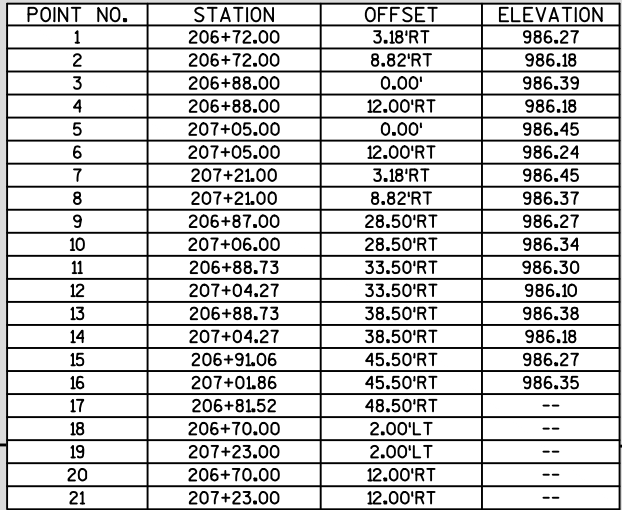
STA 206+29, RT

GENERAL NOTES














- 1) DRAWINGS NOT TO SCALE
- 2) DESIGN NEW OVERHEAD SIGN SUPPORTS ACCORDING TO THE AASHTO "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINARIES, AND TRAFFIC SIGNALS.", 6TH EDITION AND INTERIM REVISIONS.
 - FATIGUE LOADS ARE NOT APPLIED
- 3) PROVIDE AN IDENTIFICATION PLAQUE FOR THE OVERHEAD SIGN SUPPORTS, AS SPECIFIED IN STANDARD SPEC 641.3.1.4 AND SHOWN IN SDD "STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES, OVERHEAD SIGN SUPPORTS AND TRAFFIC SIGNALS". THE IDENTIFICATION PLAQUE IS TO BE CONSIDERED INCIDENTAL TO THE BID ITEM "OVERHEAD SIGN SUPPORT".
- 4) SIZE THE ANCHOR ROD/TEMPLATE ASSEMBLY TO FIT WITHIN THE BAR CAGE OF THE FOOTING BASE SHOWN IN THE CONTRACT PLANS IN ADDITION TO MEETING ALL APPLICABLE DESIGN REQUIREMENTS FOR THE DESIGN OF THE UPRIGHT BASE CONNECTION. SEE SDD "36-INCH DIAMETER FULL SPAN OVERHEAD SIGN SUPPORT BASE" FOR ADDITIONAL INFORMATION.
- 5) DESIGN THE STRUCTURE BASE PLATE CONNECTION TO ACCOMMODATE A MINIMUM OF SIX (6) ANCHOR RODS.
- 6) PROVIDE DESIGN CALCULATIONS.
- 7) SIGNS SHALL BE INSTALLED ON THE OVERHEAD SIGN SUPPORT AT THE TIME OF ERECTION.

**OVERHEAD SIGN SUPPORT DETAIL**

S-71-33
STA 209+50
STH 54
(VIEW LOOKING DOWN STATION)



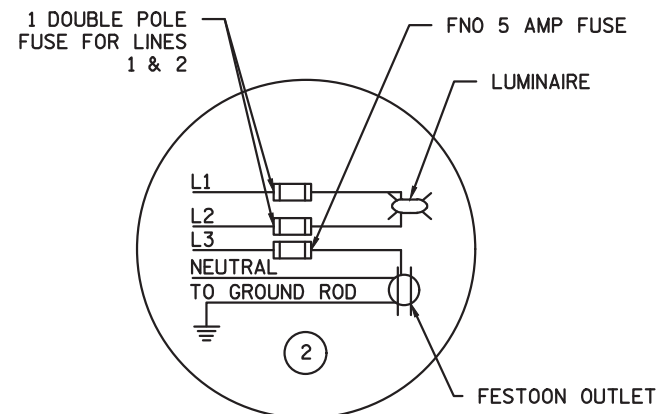
LEGEND

- | | |
|---|---|
|  | CONCRETE CURB & GUTTER 30-INCH TYPE D |
|  | CONCRETE CURB & GUTTER HES 30-INCH TYPE D |
|  | CONCRETE CURB & GUTTER 18-INCH TYPE D |
|  | CONCRETE MEDIAN SLOPED NOSE  |
|  | CONCRETE DRIVEWAY HES 6-INCH |
|  | ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES, 3-INCH |
|  | SAWING ASPHALT <u>XXXX</u> |
|  | CONCRETE SIDEWALK 4-INCH |
|  | CONCRETE SIDEWALK 6-INCH |
|  | CURB RAMP DETECTABLE WARNING FIELD (YELLOW) |
|  | 6" HMA PAVEMENT 3 MT 58-28 S AND
HMA PAVEMENT 4 MT 58-28 H (SEE TYPICAL SECTIONS) |
|  | 2" HMA PAVEMENT 4 MT 58-28 H |

NOTE

* PLACE REVERSE SLOPE GUTTER PER SDD
"CONCRETE CURB, CONCRETE CURB & GUTTER
AND TIES"

SERVICE EQUIPMENT ONE LINE DIAGRAM

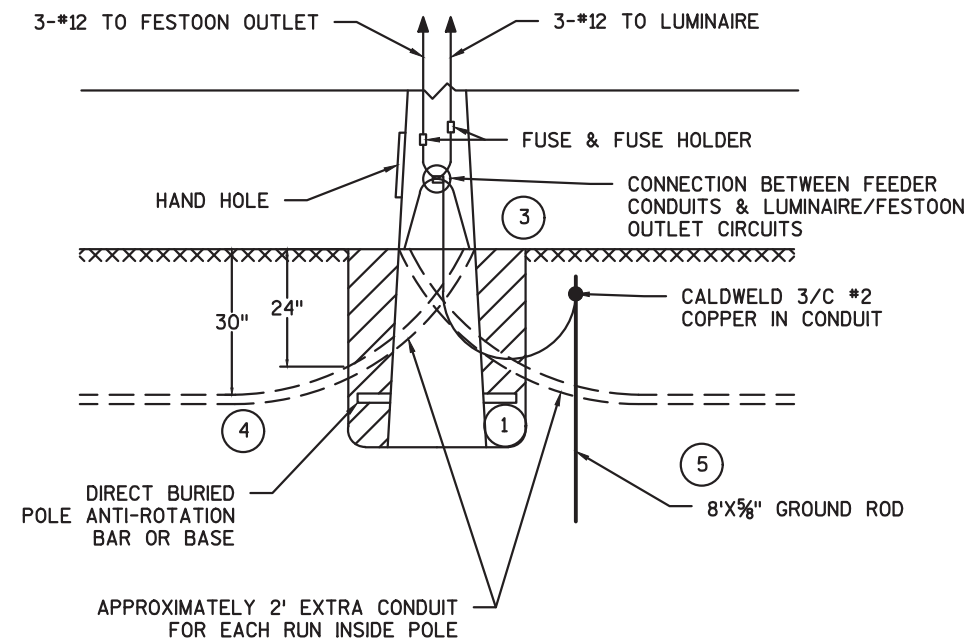


LUMINAIRE 240V
FESTOON 120V TO GROUND

- (1) ELECTRICAL CONTRACTOR TO REINSTALL FIBERGLASS POLES PER SPECIFICATIONS, POLES TO BE DIRECT BURIED 6'0" (OR TO MANUFACTURERS RECOMMENDATION) IN FIRM SOIL. POLE TO BE LEVELED AND TAMPED WITH WASHED GRAVEL OR DECOMPOSED GRANITE.
- (2) ELECTRICAL CONTRACTOR TO FURNISH AND INSTALL 3-#12 COPPER CONDUCTORS FROM MAIN LINE TO FESTOON OUTLET AND 3-#12 COPPER CONDUCTORS FROM MAINLINE TO LUMINAIRE. ELECTRICAL CONTRACTOR TO PROVIDE AND INSTALL WATERTIGHT IN-LINE FUSE HOLDER AND FNO 5 AMP CARTRIDGE FUSE FOR EACH FESTOON OUTLET CIRCUIT AND EACH LIGHTING CIRCUIT. CONNECT THE LIGHT CIRCUIT TO LINES 1 & 2 AND THE FESTOON OUTLET CIRCUIT TO LINE 3.
- (3) ELECTRICAL CONTRACTOR TO SUPPLY AND INSTALL TWO COPPER SPLIT BOLT CONNECTORS PER CONNECTION FOR LUMINAIRE CIRCUIT CONNECTION AND FOR FESTOON OUTLET CIRCUIT CONNECTION. WATER PROOF AND INSULATE THE CONNECTIONS WITH ELECTRICAL TAPE.
- (4) ELECTRICAL CONTRACTOR TO SUPPLY AND INSTALL MAIN FEEDERS IN RIGID PVC CONDUIT, PER SPECIFICATIONS.
- (5) ELECTRICAL CONTRACTOR TO SUPPLY AND INSTALL ONE 8'X5/8" COPPER CLAD GROUND ROD PER POLE. CONNECT GROUND ROD TO NEUTRAL CONDUCTOR OF THE FEEDER CIRCUIT WITH #6 BARE COPPER WIRE.

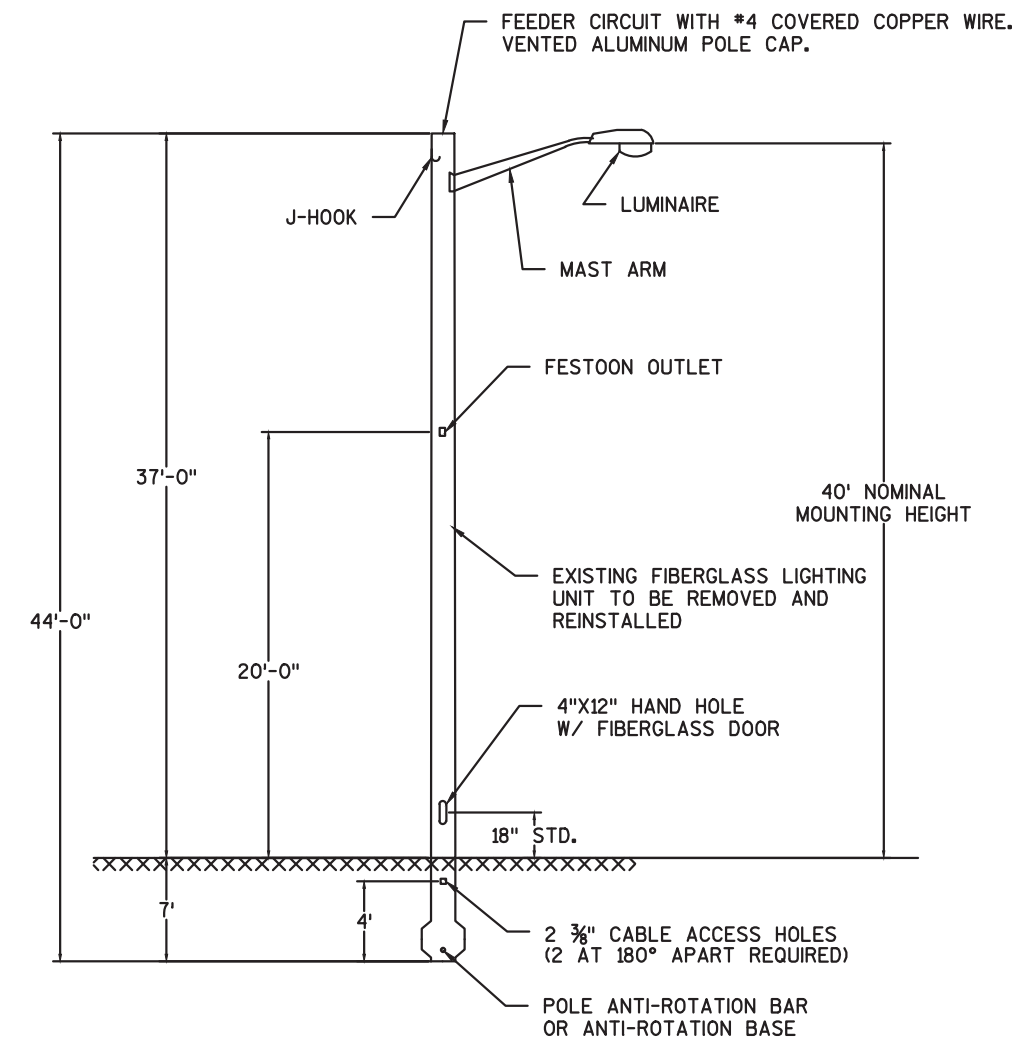
CONDUIT ENTRANCE DETAIL

NOT TO SCALE



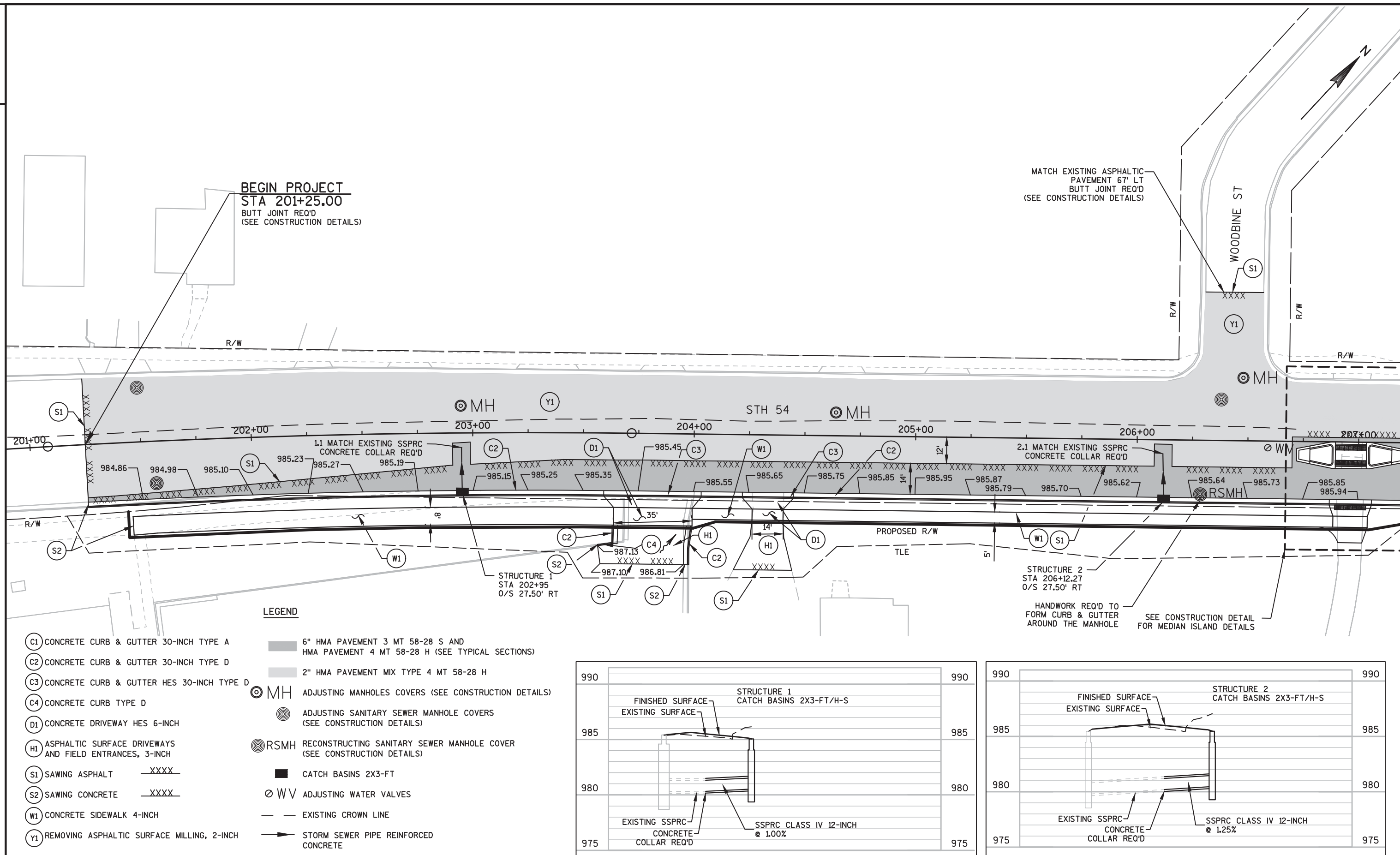
GENERAL NOTES

1. 4" X 12" REINFORCED HANDHOLE & COVER ASSEMBLY WITH 2 (TWO) 1/4" X 3/4" - 20 TPI HEX HEAD STAINLESS STEEL BOLTS.
2. GROMMETS, 1" CHASE NIPPLES OR 1" CLOSE CONDUIT NIPPLES WITH BUSHINGS SHALL BE PROVIDED FOR 1 3/8" HOLE IN POLE SHAFT FOR WIRING.
3. REINSTALL VENTILATED, CAST, METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH 1 (ONE) 1/4" X 3/4" TPI STAINLESS STEEL, HEX HEAD BOLT.



REMOVE AND REINSTALL STREET LIGHT ASSEMBLY DETAIL

NOT TO SCALE



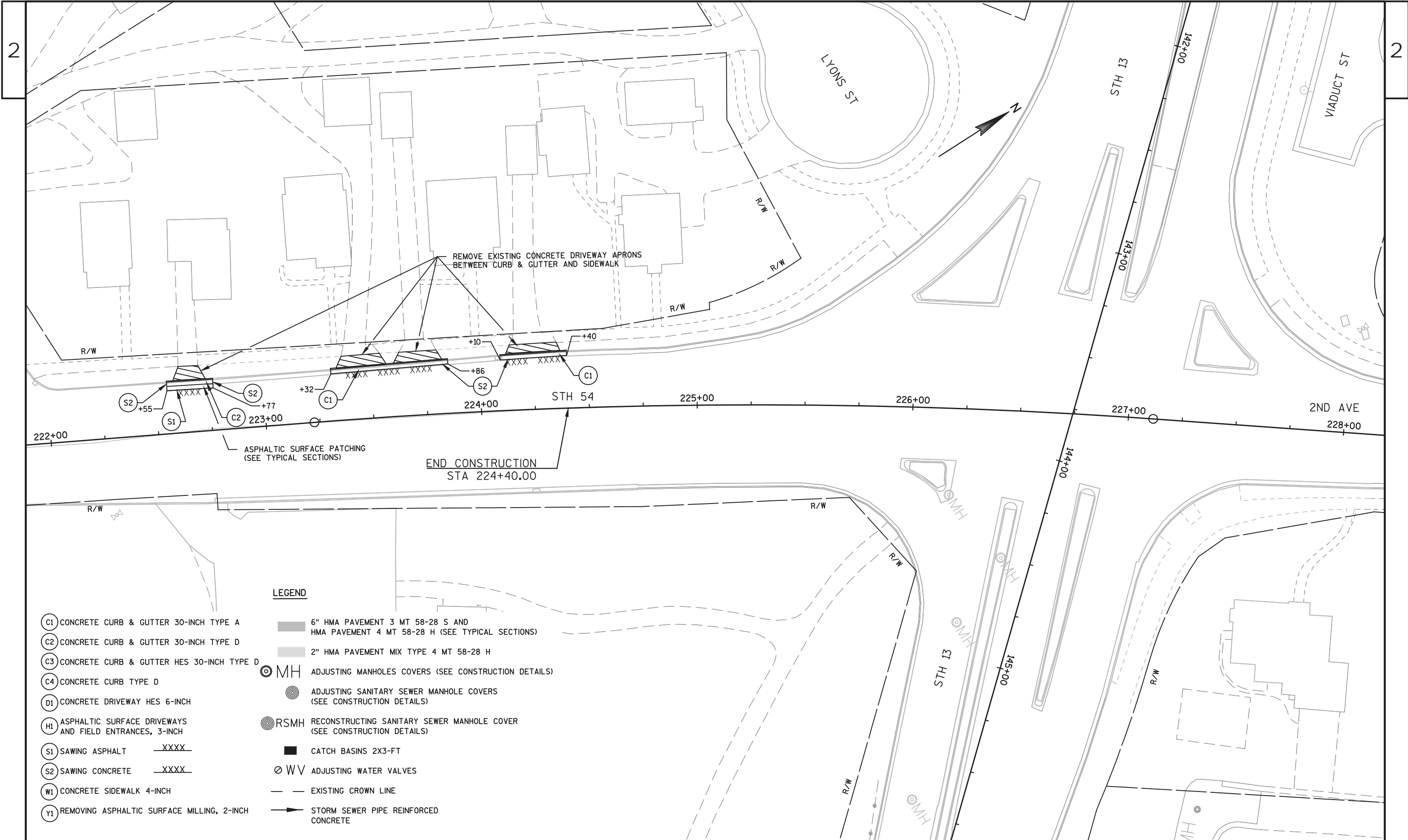
2

- | | | |
|-----|---|-----|
| 990 | <p>STRUCTURE 3
CATCH BASINS 2X3-FT/H-S</p> <p>FINISHED SURFACE</p> <p>EXISTING SURFACE</p> <p>EXISTING SSPRC</p> <p>CONCRETE
COLLAR REQ'D</p> <p>SSPRC CLASS IV 12-INCH
Ø 1.60%</p> | 990 |
| 985 | | 985 |
| 980 | | 980 |
| 975 | | 975 |



STRUCT NO.	STATION	(1) OFFSET	C-C (FT)	TO PIPE	CATCH BASIN TYPE / COVER	(2) RIM/ GRATE ELEV .	(3) T.O.S. ELEV .	(4) DEPTH (FT)	DISCHARGE PIPE						REMARKS
									CLASS	SIZE (IN)	DISCHARGE ELEV .	PIPE CONNECTION ELEV .	(5) LENGTH (FT)	(6) SLOPE (%)	
1	202+95.00	27.50' RT	21.0	1.1	2x3-FT / H-S	984.93	984.09	5.56	IV	12	980.53	980.33	20	1.00	CONCRETE COLLAR REQ'D
2	206+12.27	27.50' RT	21.0	2.1	2x3-FT / H-S	985.39	984.55	6.10	IV	12	980.45	980.20	20	1.25	CONCRETE COLLAR REQ'D
3	209+87.73	17.65' RT	11.0	3.1	2x3-FT / H-S	986.04	985.20	4.21	IV	12	982.99	982.83	10	1.60	CONCRETE COLLAR REQ'D

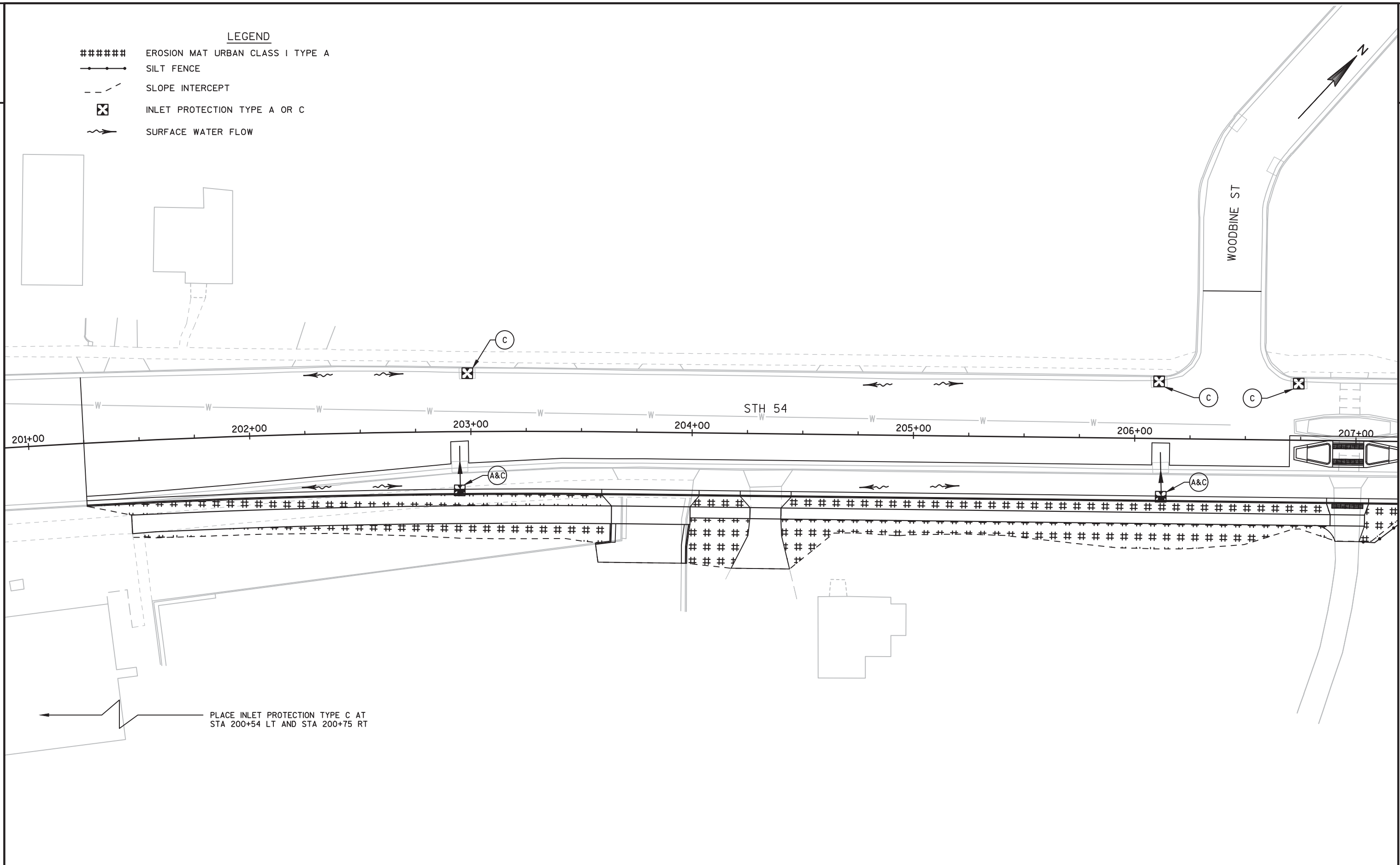
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|-----------------------|------------|-------------|----------------|-------|---|
| PROJECT NO:6999-03-70 | HWY:STH 54 | COUNTY:WOOD | PAVING DETAILS | SHEET | E |
|-----------------------|------------|-------------|----------------|-------|---|



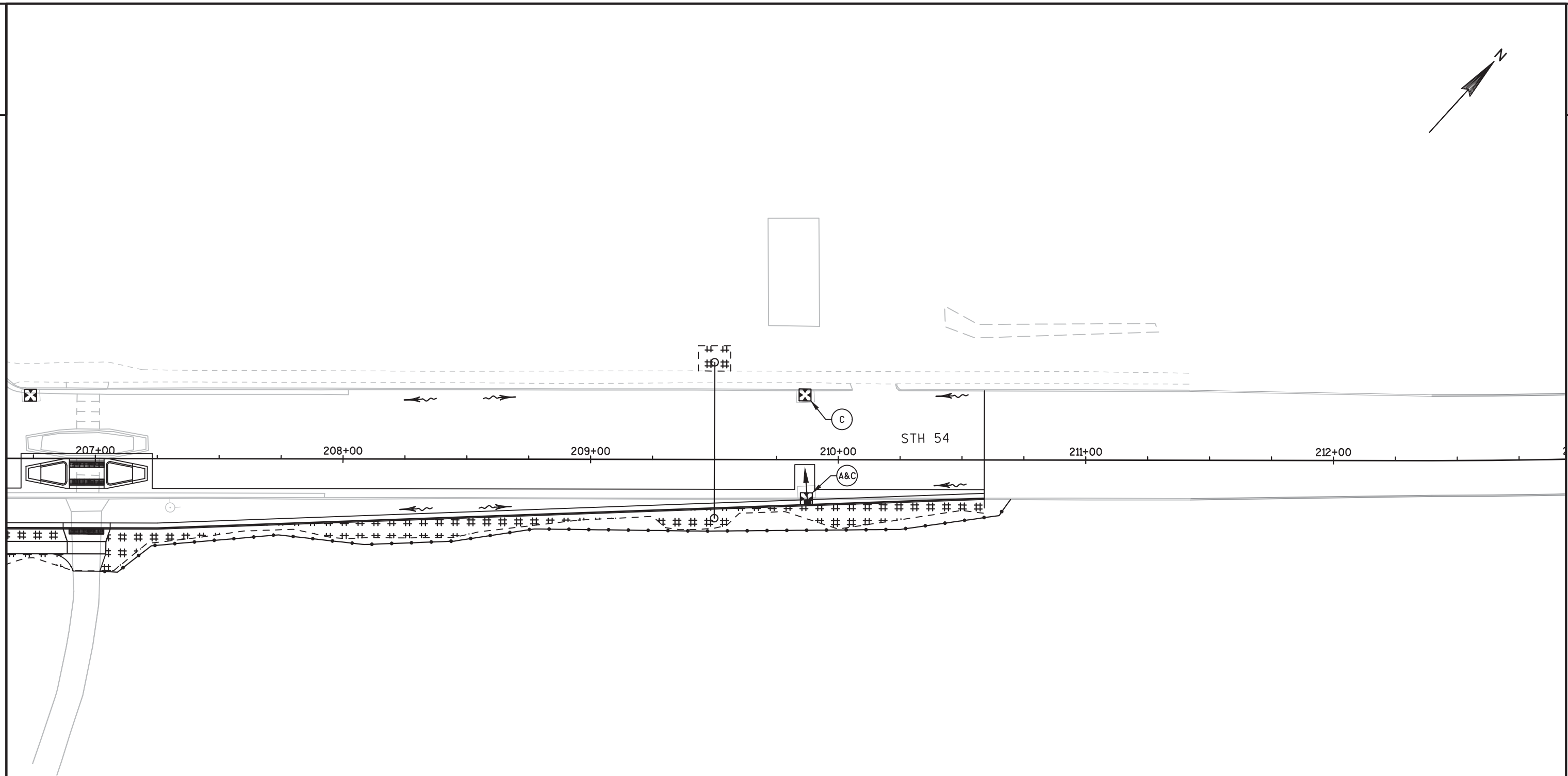
- (C1) CONCRETE CURB & GUTTER 30-INCH TYPE A
- (C2) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (C3) CONCRETE CURB & GUTTER HES 30-INCH TYPE D
- (C4) CONCRETE CURB TYPE D
- (D1) CONCRETE DRIVEWAY HES 6-INCH
- (H1) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES, 3-INCH
- (S1) SAWING ASPHALT XXXX
- (S2) SAWING CONCRETE XXXX
- (W1) CONCRETE SIDEWALK 4-INCH
- (Y1) REMOVING ASPHALTIC SURFACE MILLING, 2-INCH

- LEGEND**
- 6" HMA PAVEMENT 3 MT 58-28 S AND HMA PAVEMENT 4 MT 58-28 H (SEE TYPICAL SECTIONS)
 - 2" HMA PAVEMENT MIX TYPE 4 MT 58-28 H
 - ⊙ MH ADJUSTING MANHOLES COVERS (SEE CONSTRUCTION DETAILS)
 - ⊙ ADJUSTING SANITARY SEWER MANHOLE COVERS (SEE CONSTRUCTION DETAILS)
 - ⊙ RSMH RECONSTRUCTING SANITARY SEWER MANHOLE COVER (SEE CONSTRUCTION DETAILS)
 - CATCH BASINS 2X3-FT
 - ⊙ WV ADJUSTING WATER VALVES
 - EXISTING CROWN LINE
 - ➔ STORM SEWER PIPE REINFORCED CONCRETE

- LEGEND
- ##### EROSION MAT URBAN CLASS I TYPE A
 - SILT FENCE
 - - - SLOPE INTERCEPT
 - ⊗ INLET PROTECTION TYPE A OR C
 - ~> SURFACE WATER FLOW

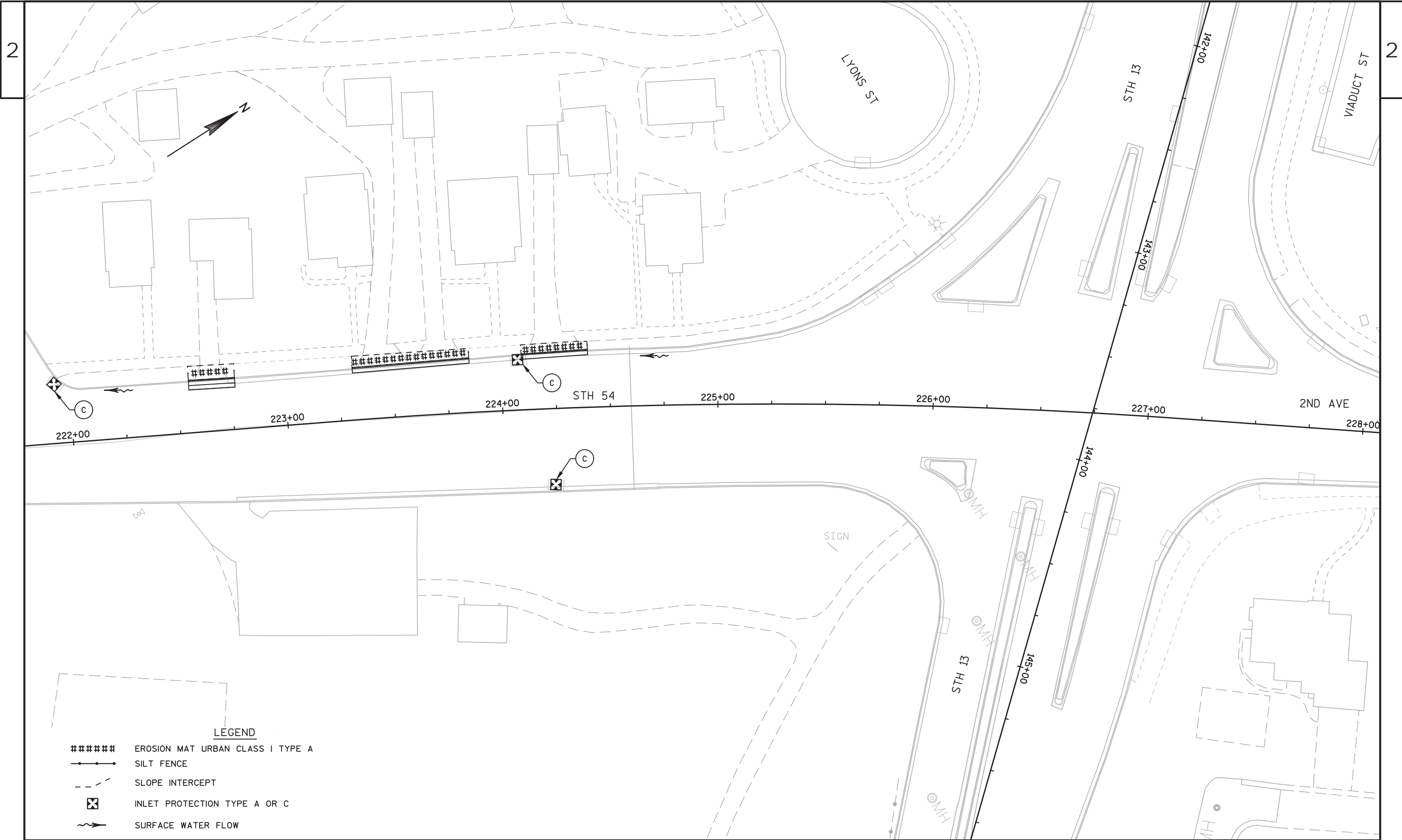


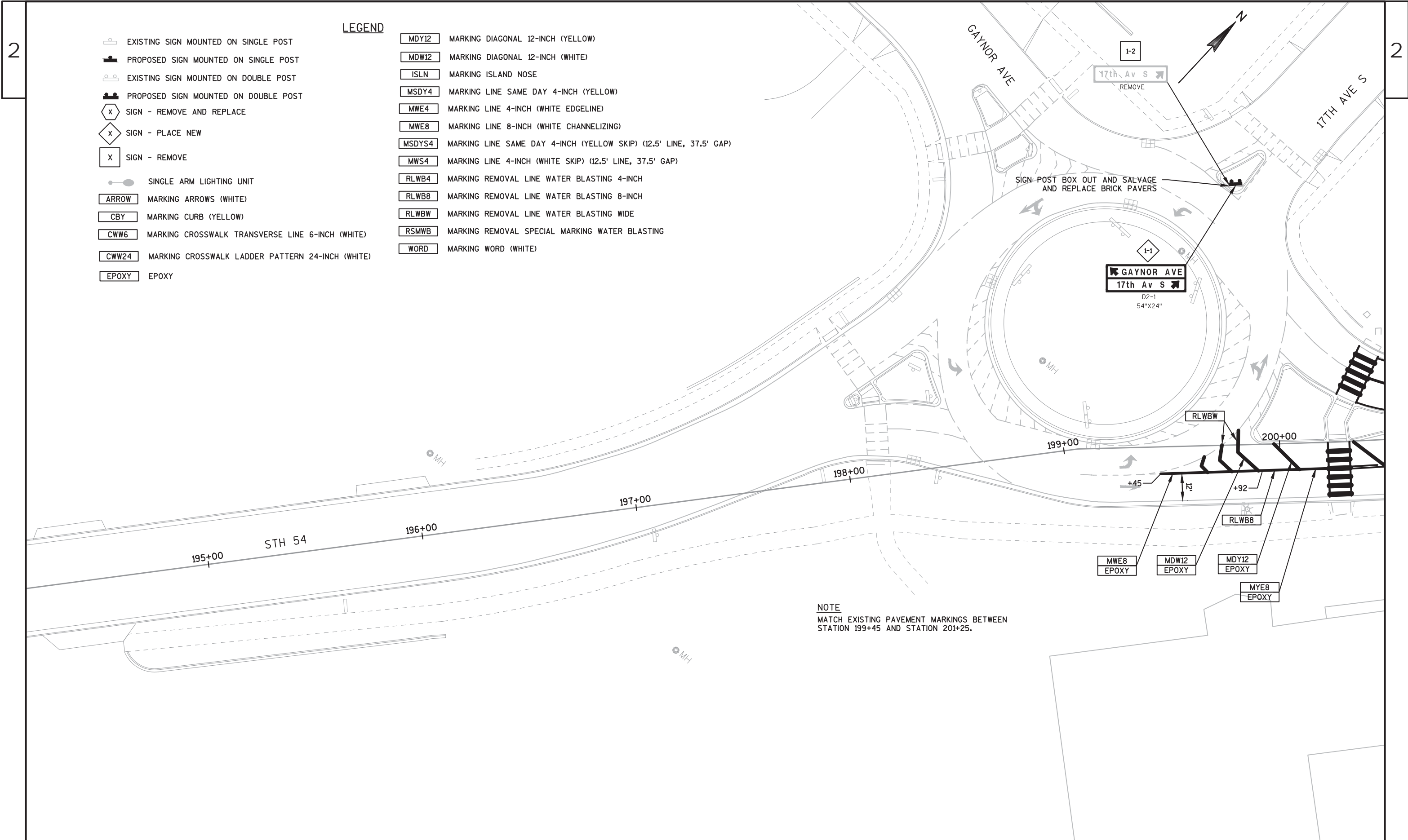
PROJECT NO:6999-03-70	HWY:STH 54	COUNTY:WOOD	EROSION CONTROL	SHEET	E
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LEGEND

- ##### EROSION MAT URBAN CLASS I TYPE A
- SILT FENCE
- - - SLOPE INTERCEPT
- ⊗ INLET PROTECTION TYPE A OR C
- ~> SURFACE WATER FLOW





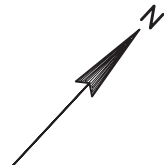
LEGEND

- EXISTING SIGN MOUNTED ON SINGLE POST
- PROPOSED SIGN MOUNTED ON SINGLE POST
- EXISTING SIGN MOUNTED ON DOUBLE POST
- PROPOSED SIGN MOUNTED ON DOUBLE POST
- SIGN - REMOVE AND REPLACE
- SIGN - PLACE NEW
- SIGN - REMOVE
- SINGLE ARM LIGHTING UNIT
- MARKING ARROWS (WHITE)
- MARKING CURB (YELLOW)
- MARKING CROSSWALK TRANSVERSE LINE 6-INCH (WHITE)
- MARKING CROSSWALK LADDER PATTERN 24-INCH (WHITE)
- EPOXY

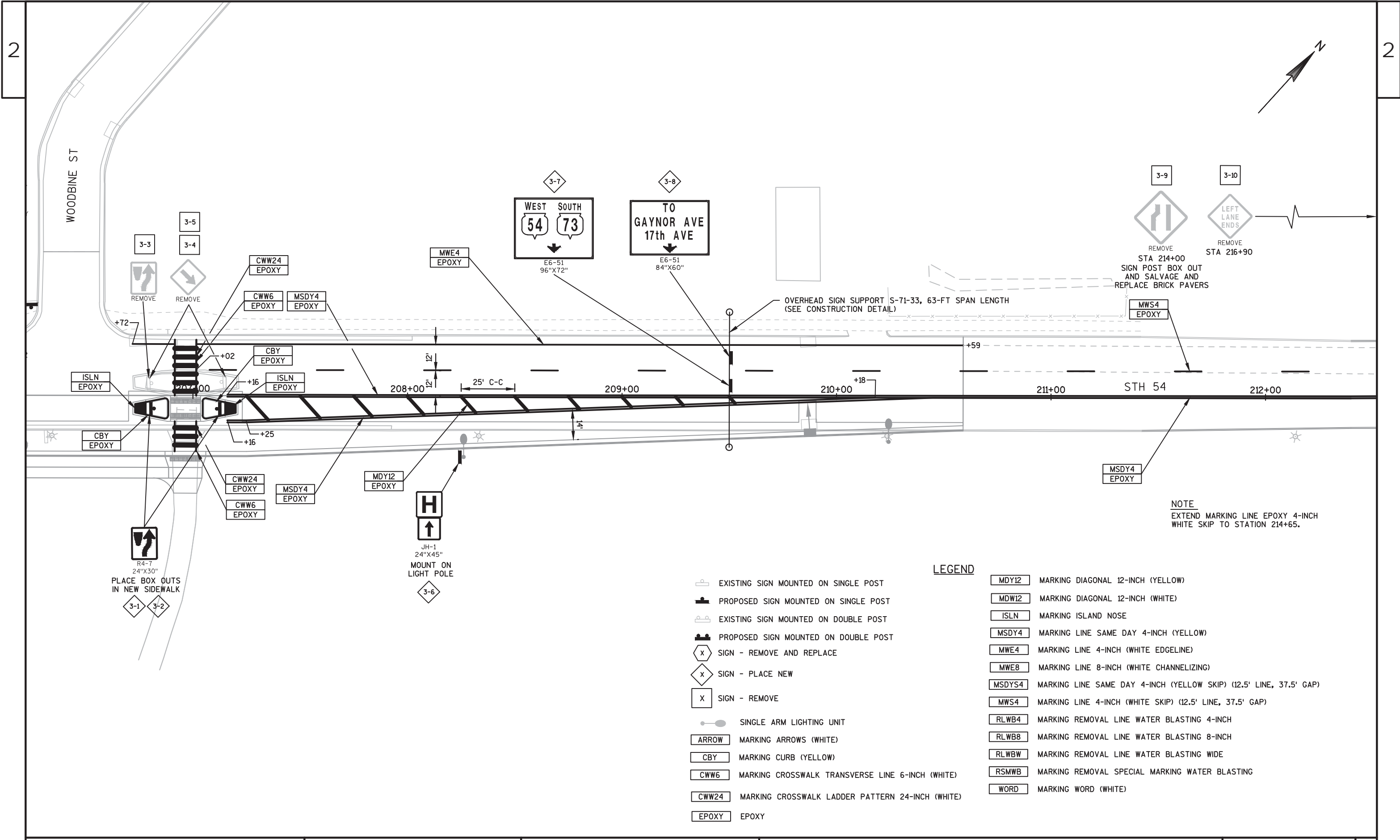
- MARKING DIAGONAL 12-INCH (YELLOW)
- MARKING DIAGONAL 12-INCH (WHITE)
- MARKING ISLAND NOSE
- MARKING LINE SAME DAY 4-INCH (YELLOW)
- MARKING LINE 4-INCH (WHITE EDGELINE)
- MARKING LINE 8-INCH (WHITE CHANNELIZING)
- MARKING LINE SAME DAY 4-INCH (YELLOW SKIP) (12.5' LINE, 37.5' GAP)
- MARKING LINE 4-INCH (WHITE SKIP) (12.5' LINE, 37.5' GAP)
- MARKING REMOVAL LINE WATER BLASTING 4-INCH
- MARKING REMOVAL LINE WATER BLASTING 8-INCH
- MARKING REMOVAL LINE WATER BLASTING WIDE
- MARKING REMOVAL SPECIAL MARKING WATER BLASTING
- MARKING WORD (WHITE)

NOTE
MATCH EXISTING PAVEMENT MARKINGS BETWEEN
STATION 199+45 AND STATION 201+25.

17TH AVE S

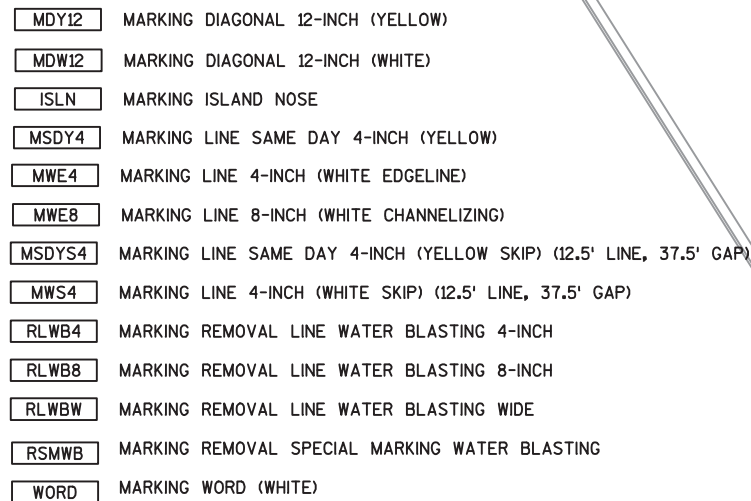


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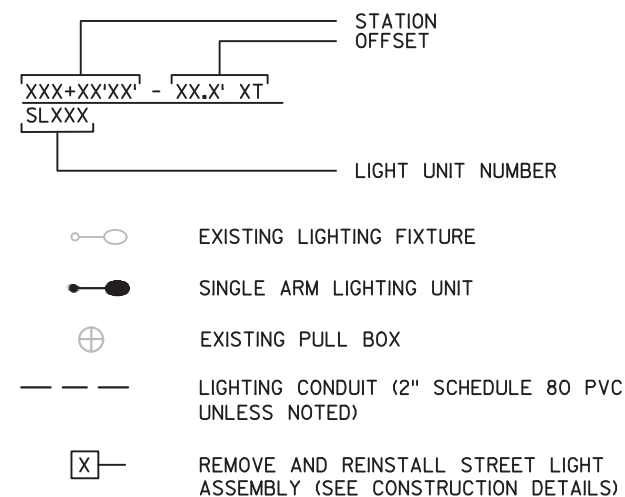
NOTE
EXTEND MARKING LINE EPOXY 4-INCH
WHITE SKIP TO STATION 214+65.

LEGEND	
	EXISTING SIGN MOUNTED ON SINGLE POST
	PROPOSED SIGN MOUNTED ON SINGLE POST
	EXISTING SIGN MOUNTED ON DOUBLE POST
	PROPOSED SIGN MOUNTED ON DOUBLE POST
	SIGN - REMOVE AND REPLACE
	SIGN - PLACE NEW
	SIGN - REMOVE
	SINGLE ARM LIGHTING UNIT
	MARKING ARROWS (WHITE)
	MARKING CURB (YELLOW)
	MARKING CROSSWALK TRANSVERSE LINE 6-INCH (WHITE)
	MARKING CROSSWALK LADDER PATTERN 24-INCH (WHITE)
	EPOXY
	MARKING DIAGONAL 12-INCH (YELLOW)
	MARKING DIAGONAL 12-INCH (WHITE)
	MARKING ISLAND NOSE
	MARKING LINE SAME DAY 4-INCH (YELLOW)
	MARKING LINE 4-INCH (WHITE EDGELINE)
	MARKING LINE 8-INCH (WHITE CHANNELIZING)
	MARKING LINE SAME DAY 4-INCH (YELLOW SKIP) (12.5' LINE, 37.5' GAP)
	MARKING LINE 4-INCH (WHITE SKIP) (12.5' LINE, 37.5' GAP)
	MARKING REMOVAL LINE WATER BLASTING 4-INCH
	MARKING REMOVAL LINE WATER BLASTING 8-INCH
	MARKING REMOVAL LINE WATER BLASTING WIDE
	MARKING REMOVAL SPECIAL MARKING WATER BLASTING
	MARKING WORD (WHITE)



CONSTRUCTION NOTES:

1. THE ENGINEER SHALL APPROVE THE FINAL LOCATION FOR ALL STREET LIGHTS IN THE FIELD PRIOR TO CONSTRUCTION.
2. THE LOCATION OF EXISTING AND PROPOSED UTILITIES AS SHOWN ON THE PLANS ARE APPROXIMATE. IN ADDITION, THERE MAY BE OTHER UTILITIES WITHIN THE PROJECT AREA WHICH ARE NOT SHOWN. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES PRIOR TO CONSTRUCTION.
3. CONDUIT RUNS SHALL CONSIST OF THE FOLLOWING 7 SINGLE CONDUCTOR WIRES EVEN IF THEY ARE NOT USED.
1 ORANGE CONDUCTOR FOR FESTOON HOT
1 WHITE CONDUCTOR (WITH ORANGE TAPE AT ENDS) FOR FESTOON NEUTRAL
2 RED CONDUCTOR FOR LIGHT HOT
1 GREEN CONDUCTOR FOR GROUND
2 BLACK CONDUCTORS FOR FUTURE USE
4. STREET LIGHTING OFFSETS TO CENTER OF LIGHT BASE SHOULD MAINTAIN 2' FROM EDGE OF SIDEWALK WHERE APPLICABLE, AND 3.5' FROM FACE OF CURB IN ALL OTHER CASES.
5. CONTRACTOR SHALL MAINTAIN UPSTREAM LIGHTING BY TEMPORARILY SPLICING CONDUCTORS UNTIL PROPOSED LIGHTING INFRASTRUCTURE IS COMPLETE AND ACTIVATED.
6. SEE CONSTRUCTION DETAILS FOR WIRING AND REMOVAL/REINSTALLATION OF THE STREET LIGHTING ASSEMBLY.

LIGHTING PLAN LEGEND:

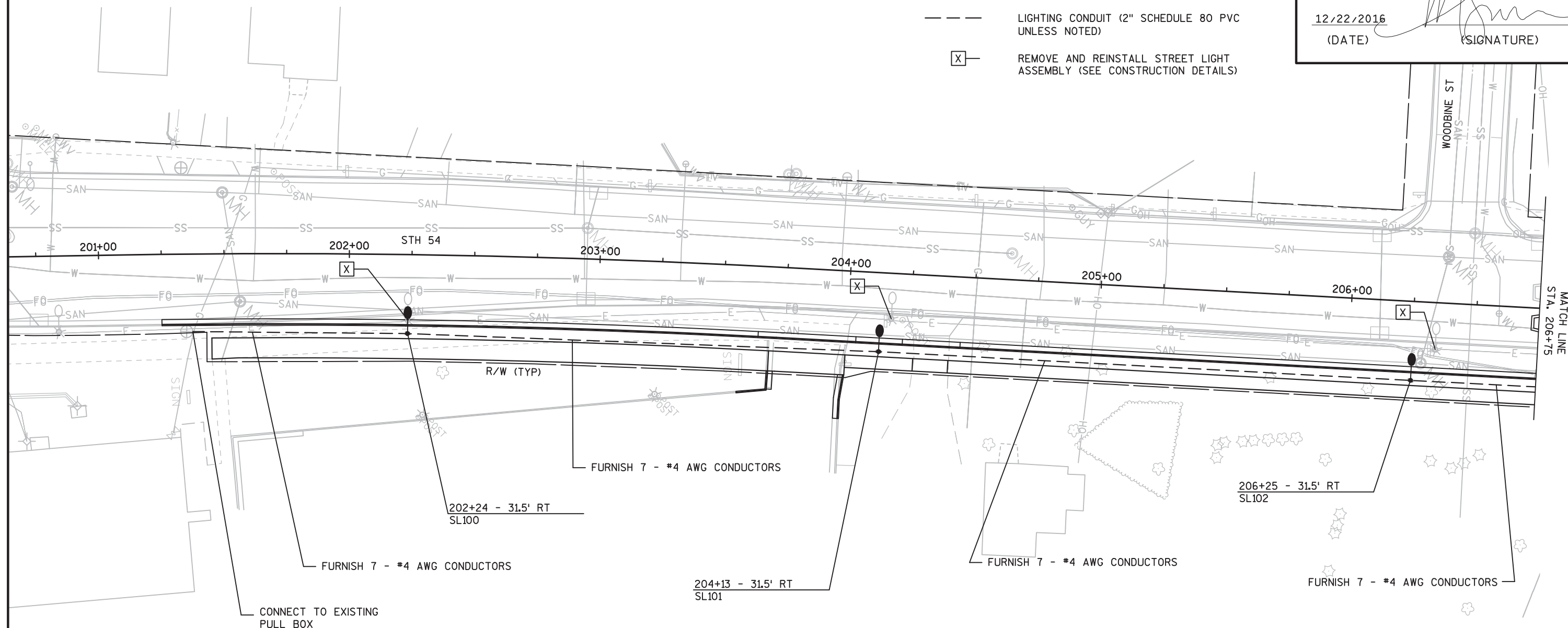
STREET LIGHTING PLANS PREPARED BY:



12/22/2016

(DATE)

(SIGNATURE)



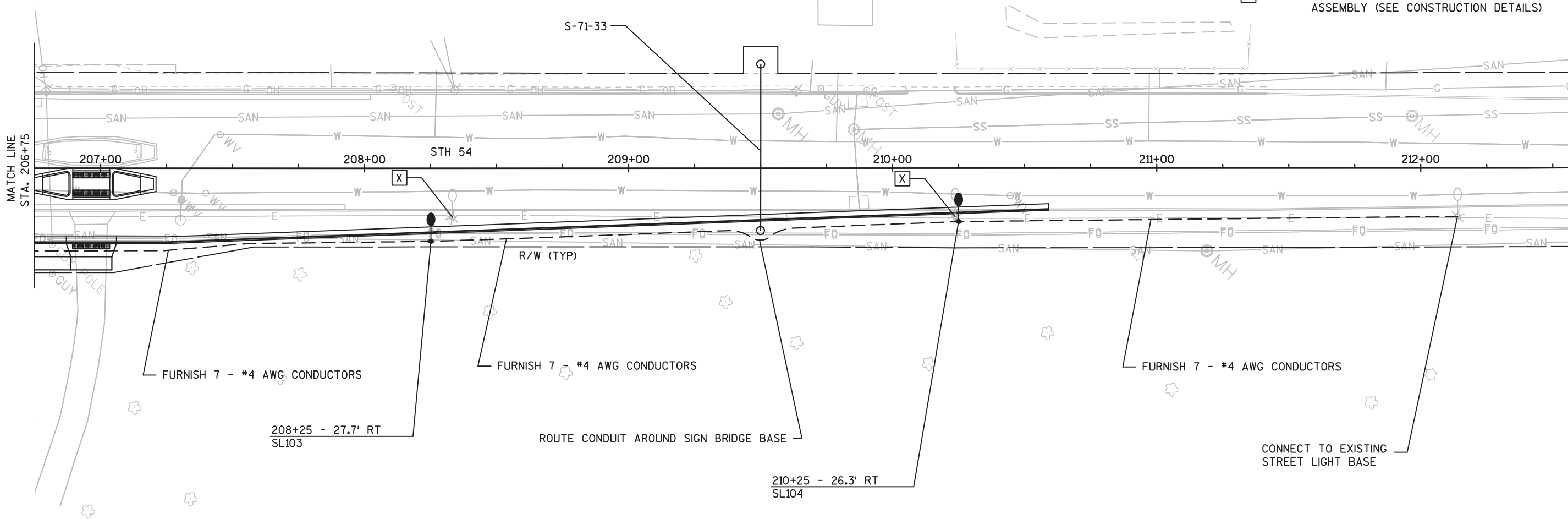
SHEET 1 OF 2

CONSTRUCTION NOTES:

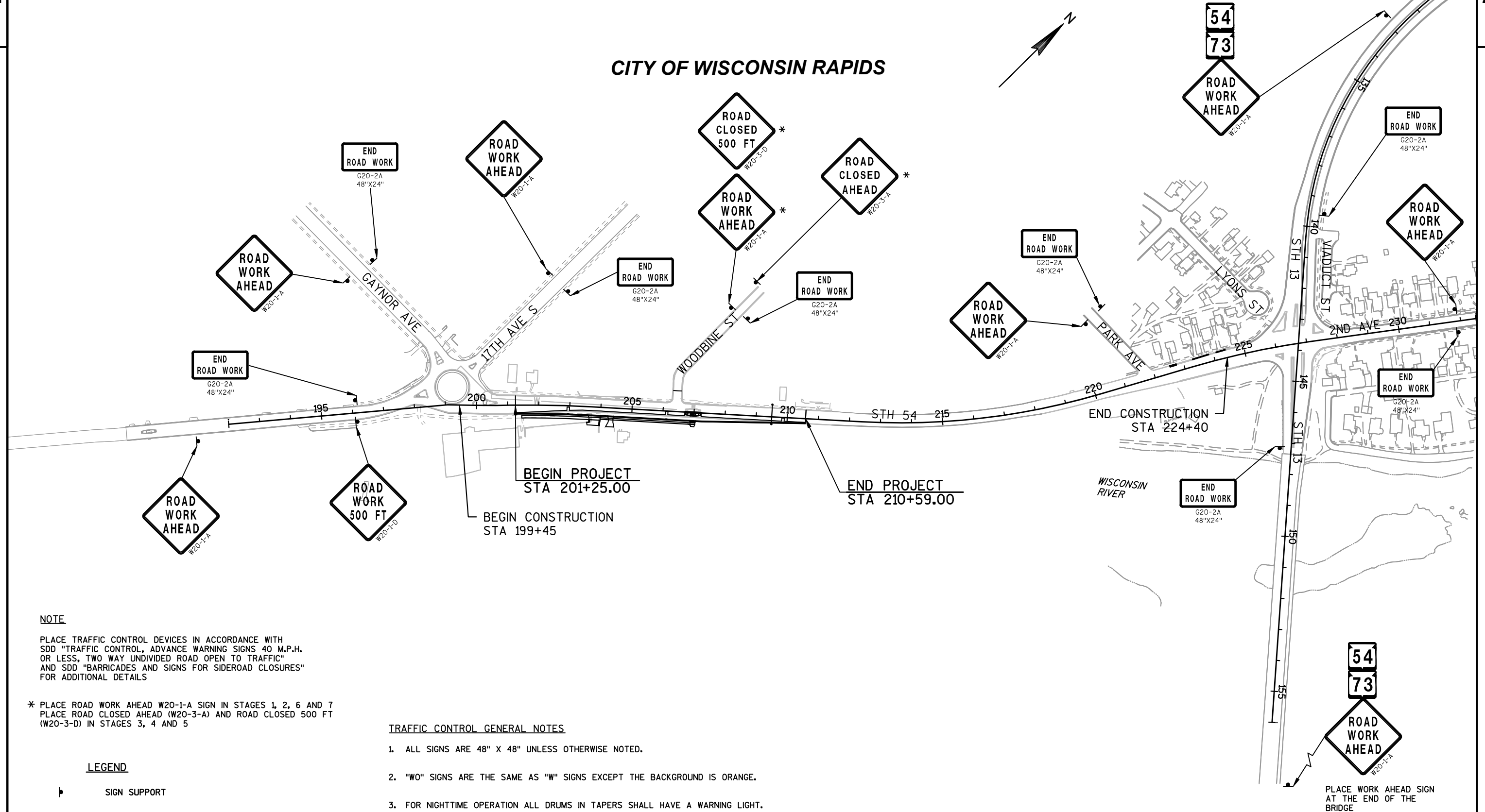
1. THE ENGINEER SHALL APPROVE THE FINAL LOCATION FOR ALL STREET LIGHTS IN THE FIELD PRIOR TO CONSTRUCTION.
2. THE LOCATION OF EXISTING AND PROPOSED UTILITIES AS SHOWN ON THE PLANS ARE APPROXIMATE. IN ADDITION, THERE MAY BE OTHER UTILITIES WITHIN THE PROJECT AREA WHICH ARE NOT SHOWN. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES PRIOR TO CONSTRUCTION.
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 - 1 GREEN CONDUCTOR FOR GROUND
 - 2 BLACK CONDUCTORS FOR FUTURE USE
4. STREET LIGHTING OFFSETS TO CENTER OF LIGHT BASE SHOULD MAINTAIN 2' FROM EDGE OF SIDEWALK WHERE APPLICABLE, AND 3.5' FROM FACE OF CURB IN ALL OTHER CASES.
5. CONTRACTOR SHALL MAINTAIN UPSTREAM LIGHTING BY TEMPORARILY SPLICING CONDUCTORS UNTIL PROPOSED LIGHTING INFRASTRUCTURE IS COMPLETE AND ACTIVATED.
6. SEE CONSTRUCTION DETAILS FOR WIRING AND REMOVAL/REINSTALLATION OF THE STREET LIGHTING ASSEMBLY.

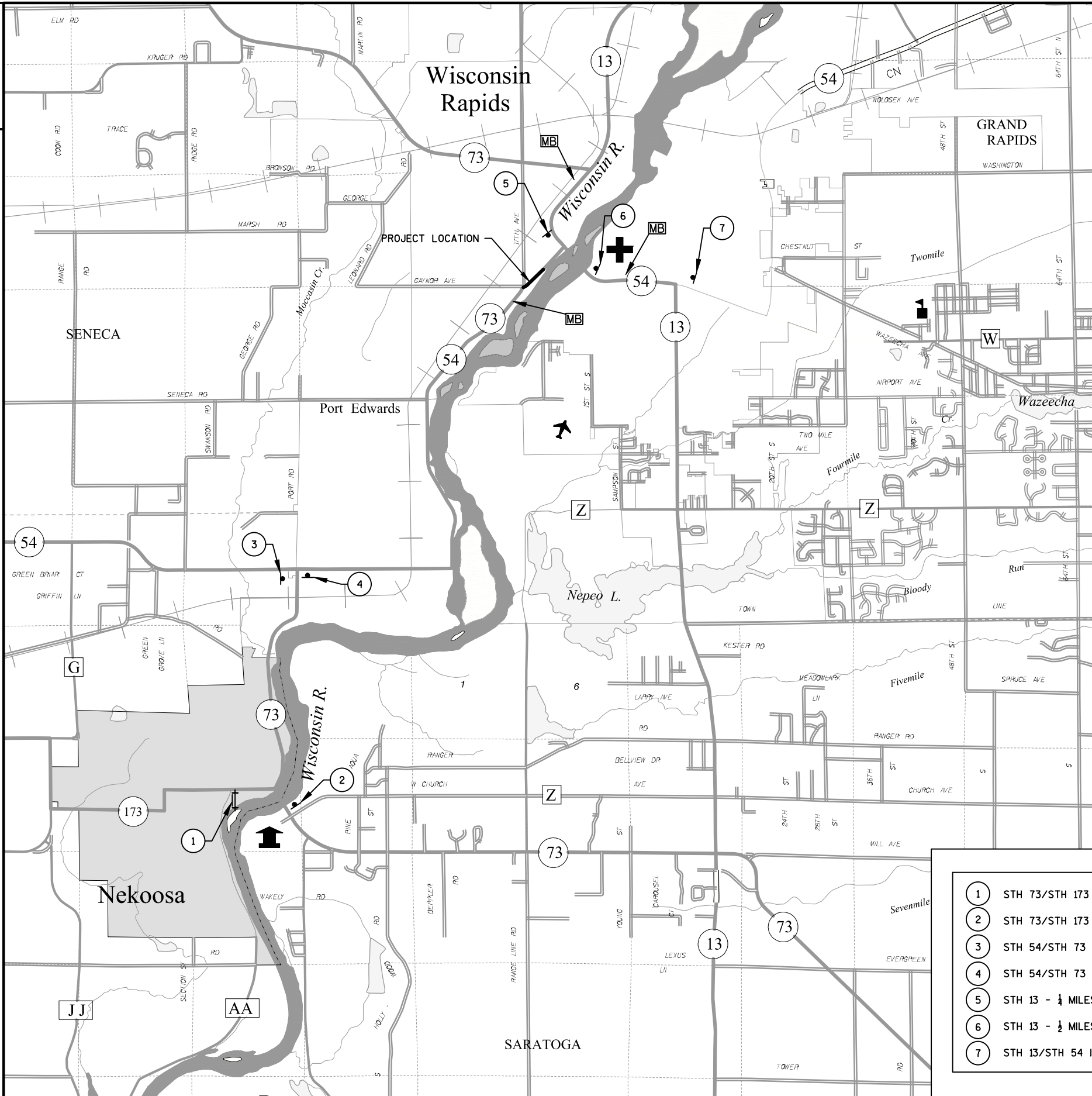
LIGHTING PLAN LEGEND:

- STATION OFFSET
XXX+XX'XX' - XX.X' XT
SLXXX
- LIGHT UNIT NUMBER
- EXISTING LIGHTING FIXTURE
- SINGLE ARM LIGHTING UNIT
- EXISTING PULL BOX
- LIGHTING CONDUIT (2" SCHEDULE 80 PVC UNLESS NOTED)
- REMOVE AND REINSTALL STREET LIGHT ASSEMBLY (SEE CONSTRUCTION DETAILS)



CITY OF WISCONSIN RAPIDS





NOTES

POST THE WIDTH RESTRICTION ADVANCED WARNING SIGNS AT THE LOCATIONS PROVIDED

ALL TRAFFIC CONTROL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED

* PLACED SIGN M1-6 WHEN ASSEMBLY IS MOUNTED ON ALL ROADWAYS OTHER THAN STH 54 AND STH 73

ADJUST TRAFFIC CONTROL SIGNS PCMS MESSAGE AS NEEDED BASED ON CONSTRUCTION SCHEDULE.

CONSIDER GEOMETRICS WHEN LOCATING TRAFFIC CONTROL SIGNS PCMS SO THE DRIVER HAS A CLEAR VIEW OF THE SIGN FOR A MINIMUM OF 500 FEET IN FRONT OF THE MESSAGE SIGN. PLACE TRAFFIC CONTROL SIGNS PCMS AS FAR AWAY FROM LIVE TRAFFIC LANES AS POSSIBLE WITHOUT HAMPERING VISIBILITY.

PLACE TRAFFIC CONTROL SIGNS PCMS AND DISPLAY THE MESSAGE 7 DAYS PRIOR TO THE EXPECTED START OF THE PROPOSED WORK.

LEGEND

- SIGN MOUNTED ON BARRICADE
- SIGN MOUNTED ON SUPPORT
- TRAFFIC CONTROL SIGNS PCMS

*

*

*

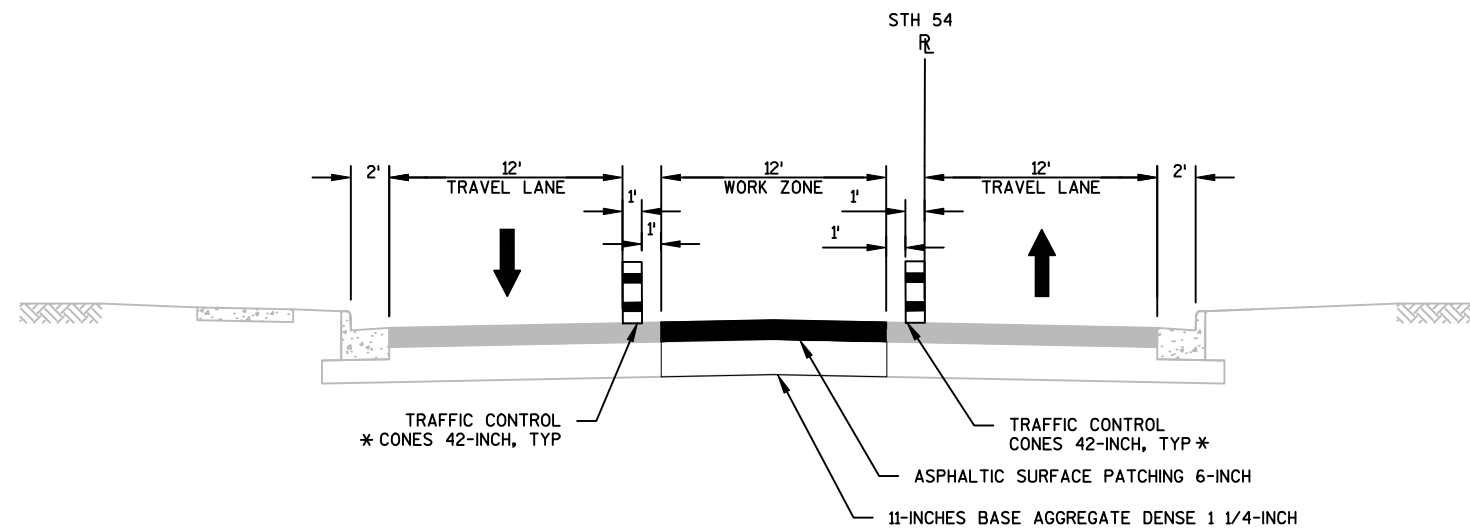
*

PLACE ABOVE SIGNS AT ALL LOCATIONS DURING STAGE 1, STAGE 2, STAGE 6 AND STAGE 7 AND AT LOCATIONS 5, 6 AND 7 DURING STAGE 3

PLACE ABOVE SIGNS DURING STAGE 3 AT LOCATIONS 1, 2, 3, AND 4

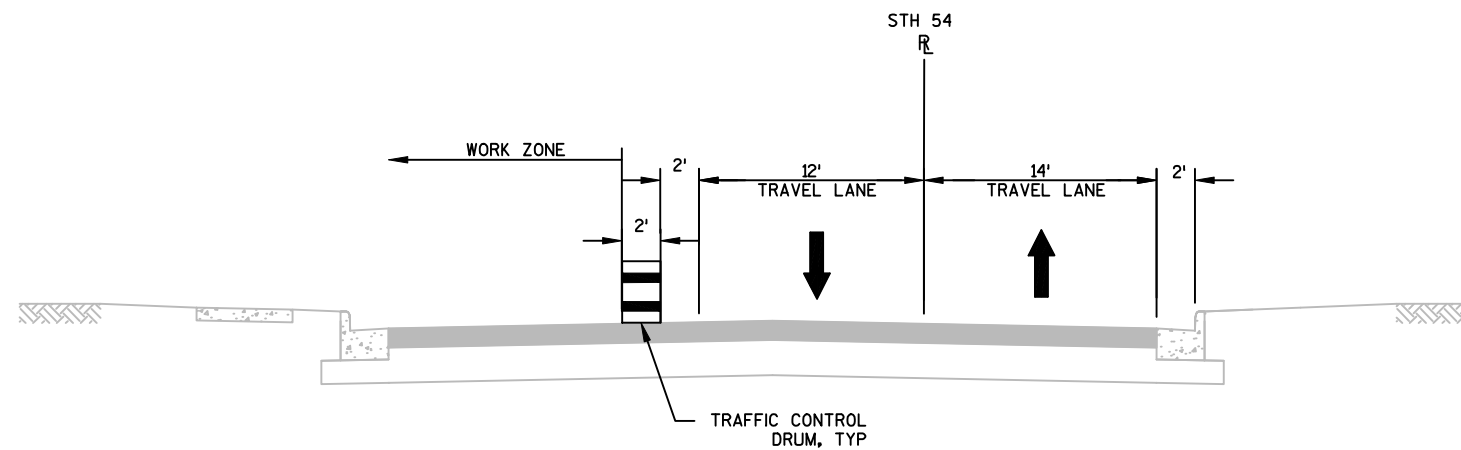
- 1 STH 73/STH 173 INTERSECTION - 6 ½ MILES AHEAD
- 2 STH 73/STH 173 INTERSECTION - 6 ½ MILES AHEAD
- 3 STH 54/STH 73 INTERSECTION - 4 ½ MILES AHEAD
- 4 STH 54/STH 73 INTERSECTION - 4 ½ MILES AHEAD
- 5 STH 13 - ¼ MILES AHEAD
- 6 STH 13 - ½ MILES AHEAD
- 7 STH 13/STH 54 INTERSECTION - 1 ¼ MILES AHEAD

TRAFFIC CONTROL SIGNS PCMS MESSAGES				
PCMS SIGN LOCATION	PRIOR TO CONSTRUCTION		ROLLING CLOSURE OVERHEAD SIGN SUPPORT INSTALLATION	
	PHASE 1 (2 SEC)	PHASE 2 (2 SEC)	PHASE 1 (2 SEC)	PHASE 2 (2 SEC)
STH 13 SB 0.4 MILES SOUTH OF STH 73	WIS54/73 ROADWORK BEGINS	# STARTING DATE	TRAFFIC STOPPED WIS54/73	BE PREPARED TO STOP
STH 13 NB 0.1 MILE NORTH OF STH 13/STH 54 INTERSECTION	WIS54/73 ROADWORK BEGINS	# STARTING DATE	TRAFFIC STOPPED WIS54/73	BE PREPARED TO STOP
STH 54/STH 73 0.5 MILES SOUTH OF GAYNOR AVENUE	WIS54/73 ROADWORK BEGINS	# STARTING DATE	TRAFFIC STOPPED AHEAD	BE PREPARED TO STOP



STAGE 1 TYPICAL SECTION
STH 54

REMOVE EXISTING PEDESTRIAN REFUGE ISLAND
STA 206+71 - STA 207+21



STAGE 2 TYPICAL SECTION
STH 54

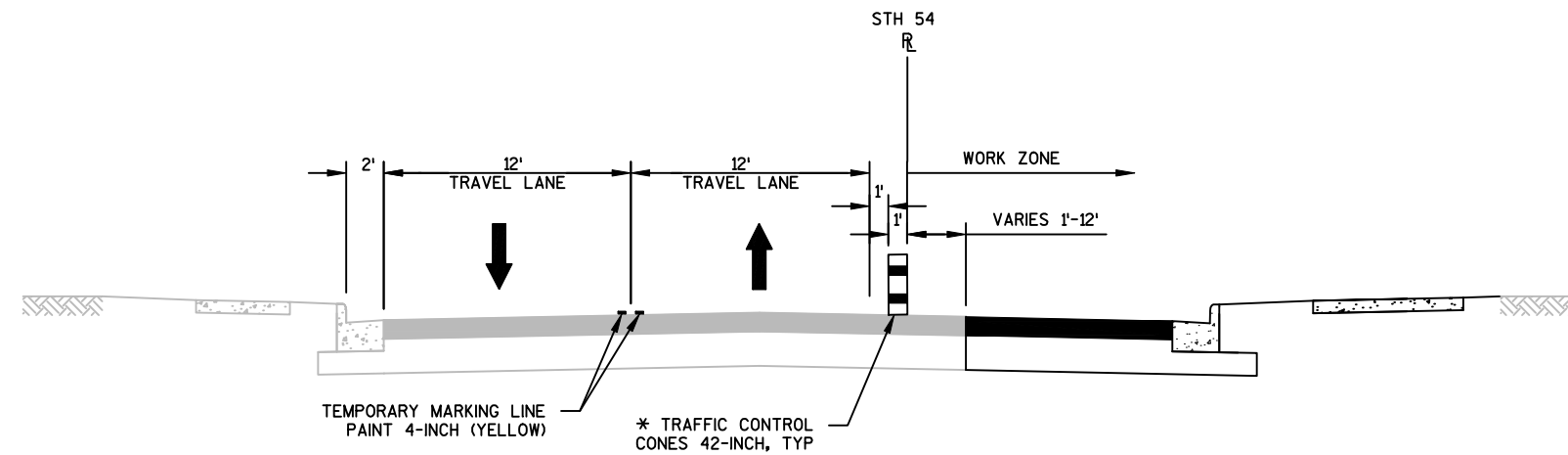
CONSTRUCT LT OVERHEAD SIGN SUPPORT
STA 209+50

NOTES

PLACE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON FREEWAY/EXPRESSWAY", "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" AND "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION".

FLAGGING TO ONE LANE CONDITIONS MAY OCCUR DURING WORKING HOURS.

* ONLY USE TRAFFIC CONTROL DRUMS IN TAPERS.



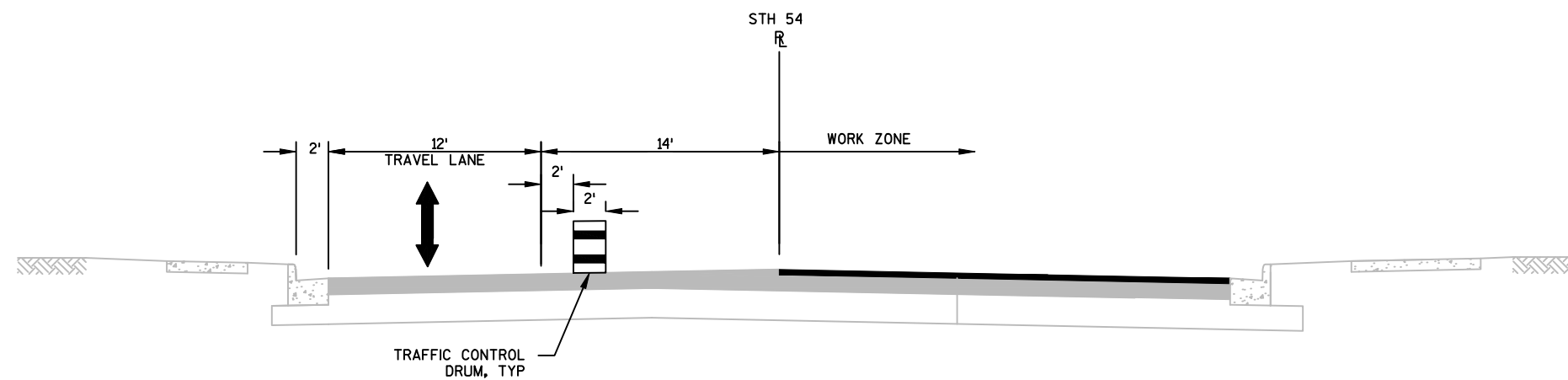
STAGE 3 TYPICAL SECTION
STH 54
CONSTRUCT PAVEMENT WIDENING, NEW PEDESTRIAN REFUGE ISLAND
& RT OVERHEAD SIGN SUPPORT BASE
STA 201+25 - STA 210+59

NOTES

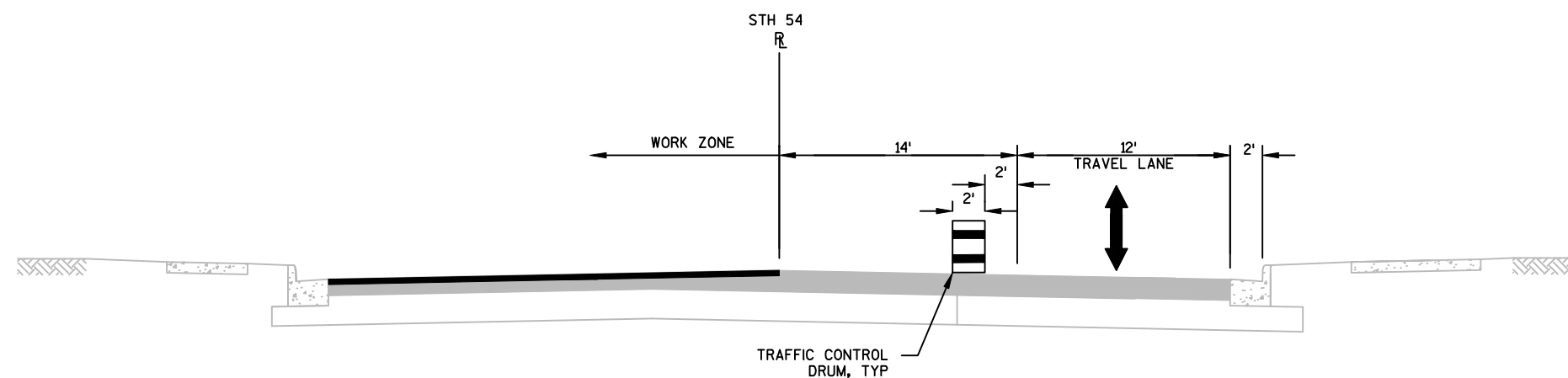
PLACE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON FREEWAY/EXPRESSWAY", "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" AND "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION".

FLAGGING TO ONE LANE CONDITIONS MAY OCCUR DURING WORKING HOURS.

* ONLY USE TRAFFIC CONTROL DRUMS IN TAPERS.



STAGE 4 TYPICAL SECTION
STH 54
 MILL AND OVERLAY EXISTING PAVEMENT (RT) &
 CONSTRUCT SURFACE LAYER OF PROPOSED WIDENING (RT)
 STA 201+25 - STA 210+59

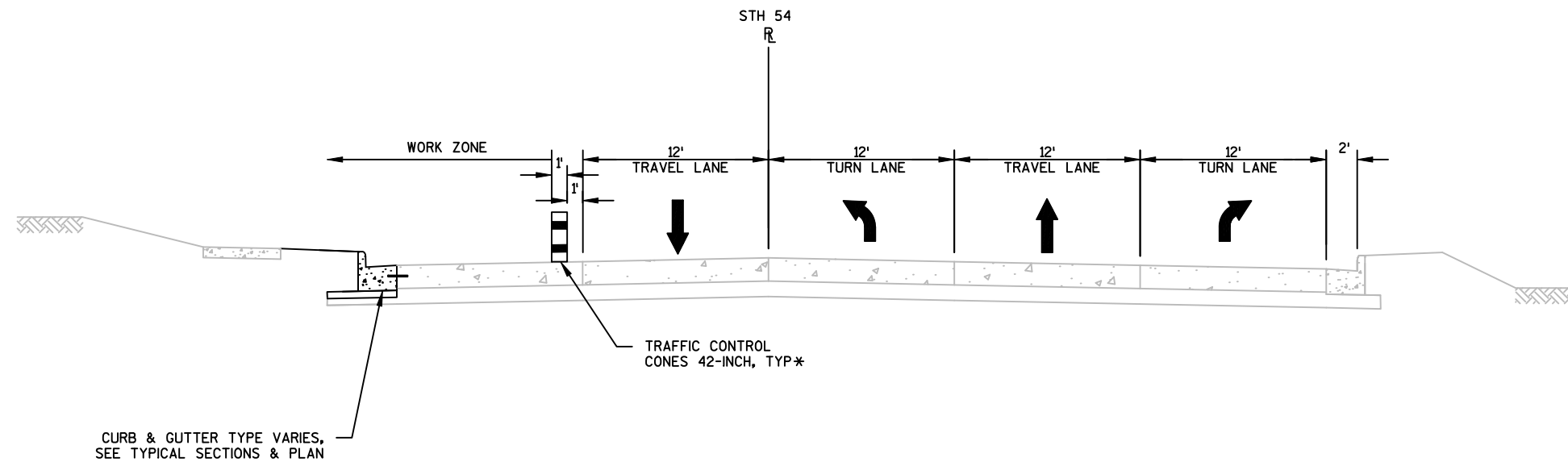


STAGE 5 TYPICAL SECTION
STH 54
 MILL AND OVERLAY EXISTING PAVEMENT (LT)
 STA 201+25 - STA 210+59

NOTES

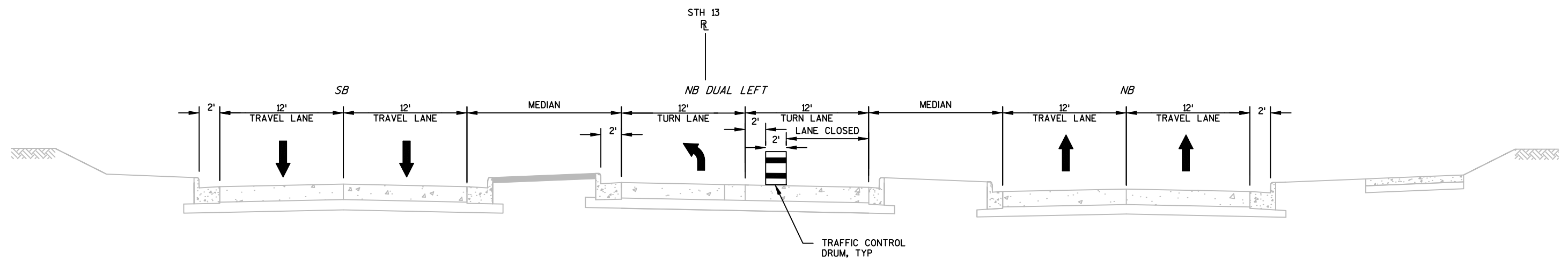
PLACE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON FREEWAY/EXPRESSWAY", "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" AND "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION".

THERE ARE NO TRAFFIC CONTROL PLAN SHEETS SHOWN FOR STAGE 4 AND STAGE 5. COMPLETE WORK UNDER FLAGGING OPERATIONS.



STAGE 6 TYPICAL SECTION
STH 54

STH 54 DRIVEWAY CLOSURES
 STA 222+55 - STA 224+40



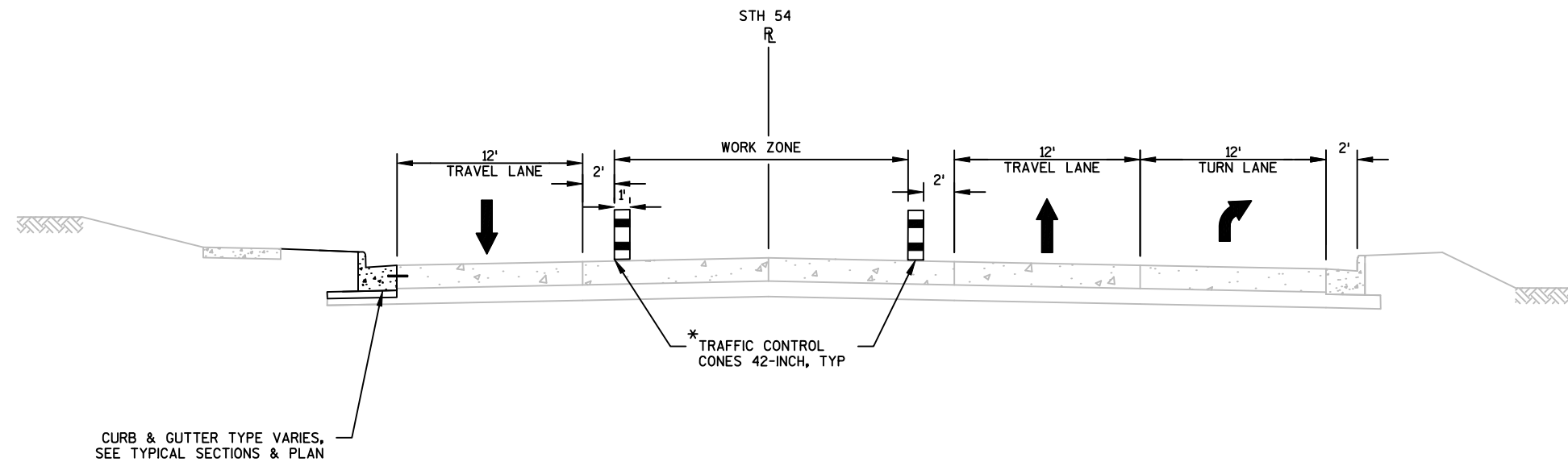
STAGE 6 (SOUTH APPROACH) TYPICAL SECTION
STH 13 (LOOKING NORTH)

STH 54 DRIVEWAY CLOSURES
 STA 222+55 - STA 224+40

NOTES

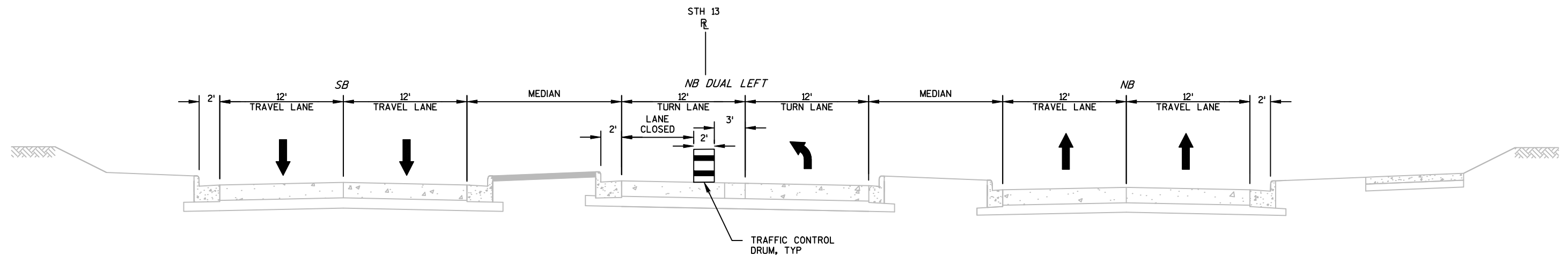
PLACE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON FREEWAY/EXPRESSWAY" AND "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY".

* ONLY USE TRAFFIC CONTROL DRUMS IN TAPERS.



STAGE 7 TYPICAL SECTION
STH 54

STH 54 PAVEMENT MARKING
STA 220+00 - STA 223+72



STAGE 7 (SOUTH APPROACH) TYPICAL SECTION
STH 13 (LOOKING NORTH)












STH 54 PAVEMENT MARKING
STA 220+00 - STA 223+72

NOTES

PLACE TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON FREEWAY/EXPRESSWAY".

* ONLY USE TRAFFIC CONTROL DRUMS IN TAPERS.

LEGEND

- 

TYPE II BARRICADE

TYPE II BARRICADE WITH ATTACHED SIGN

TYPE III BARRICADE

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM OR TRAFFIC CONTROL CONE 42-INCH,
SEE TRAFFIC CONTROL TYPICAL SECTIONS AND
MISCELLANEOUS QUANTITIES FOR LOCATIONS








TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT

SIGN MOUNTED ON SUPPORT

TRAFFIC CONTROL ARROW BOARDS

WORK ZONE / LANE CLOSURE

DIRECTION OF TRAFFIC

EXISTING PAVEMENT MARKING
- 

TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW)

TEMPORARY MARKING LINE PAINT 4-INCH (WHITE)

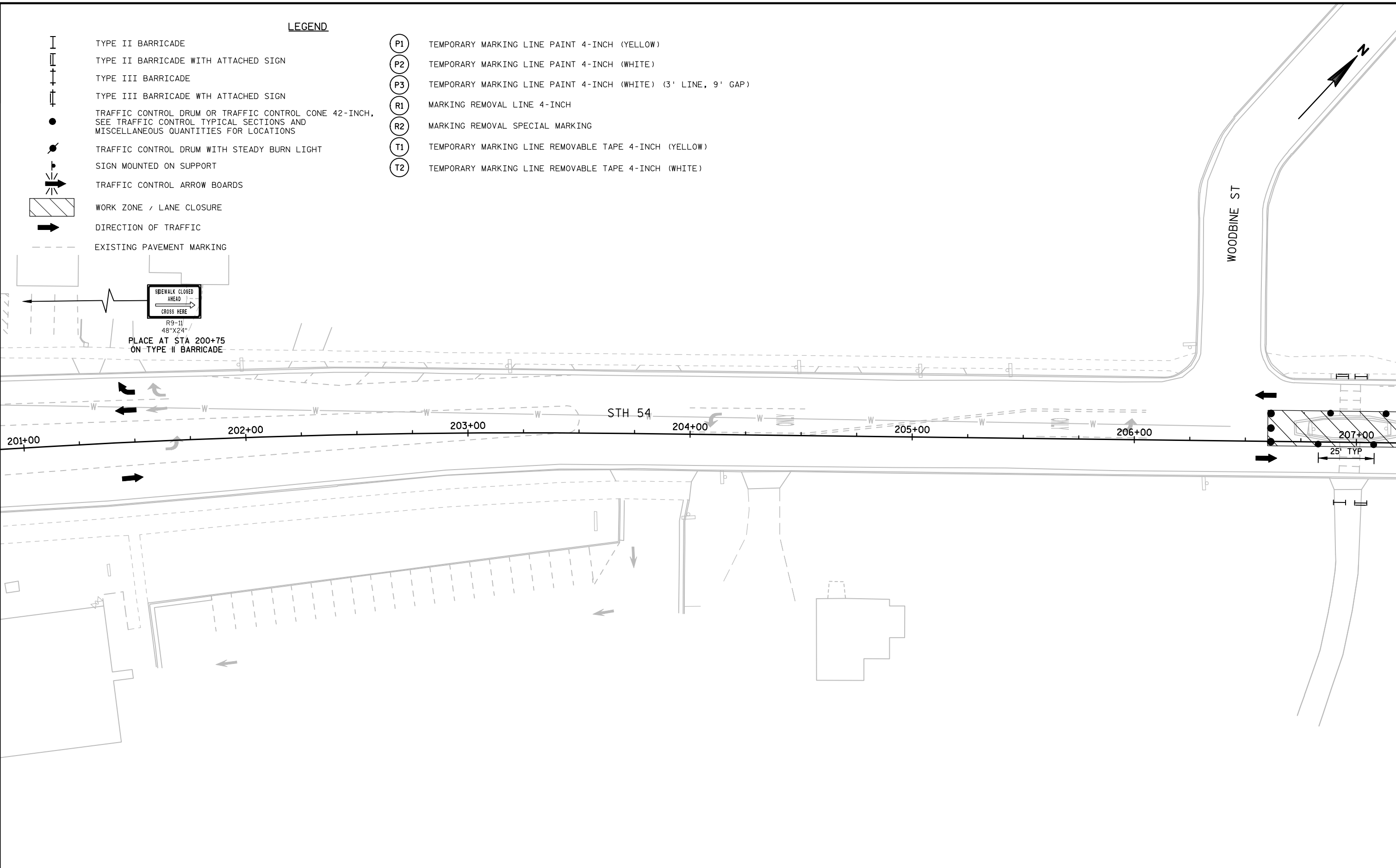
TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP)

MARKING REMOVAL LINE 4-INCH












MARKING REMOVAL SPECIAL MARKING

TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW)

TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)



LEGEND

- 

TYPE II BARRICADE

TYPE II BARRICADE WITH ATTACHED SIGN

TYPE III BARRICADE

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM OR TRAFFIC CONTROL CONE 42-INCH,
SEE TRAFFIC CONTROL TYPICAL SECTIONS AND
MISCELLANEOUS QUANTITIES FOR LOCATIONS








TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT

SIGN MOUNTED ON SUPPORT

TRAFFIC CONTROL ARROW BOARDS

WORK ZONE / LANE CLOSURE

DIRECTION OF TRAFFIC

EXISTING PAVEMENT MARKING
- 

TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW)

TEMPORARY MARKING LINE PAINT 4-INCH (WHITE)

TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP)

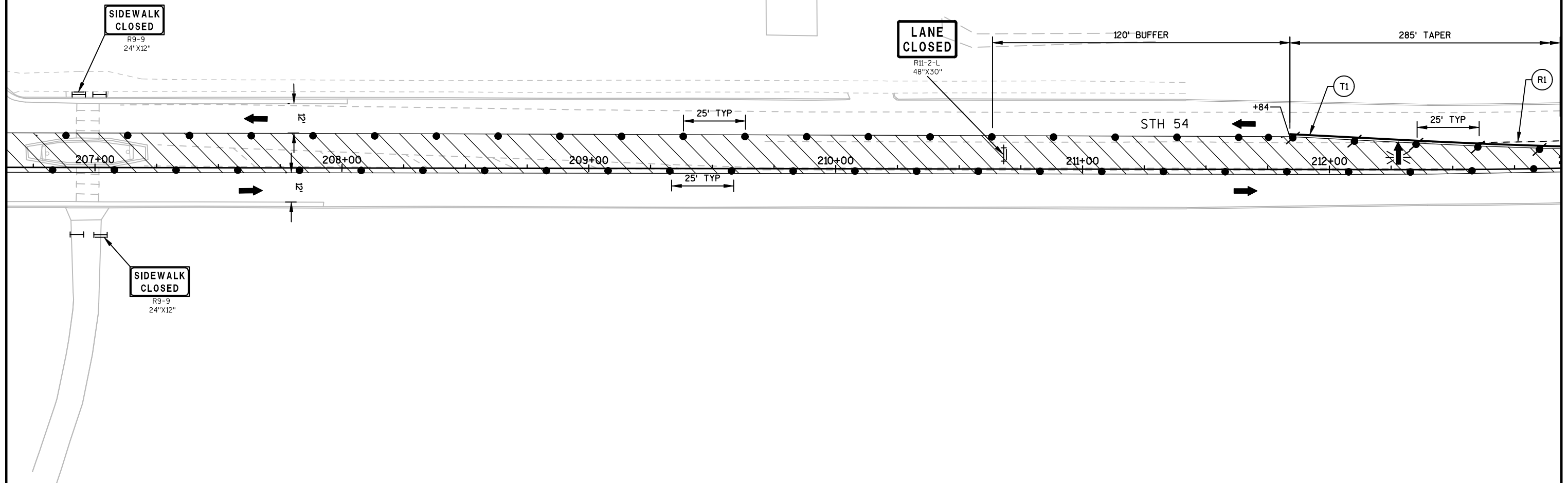
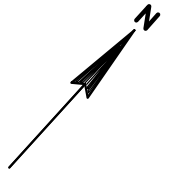
MARKING REMOVAL LINE 4-INCH

MARKING REMOVAL SPECIAL MARKING


















TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW)

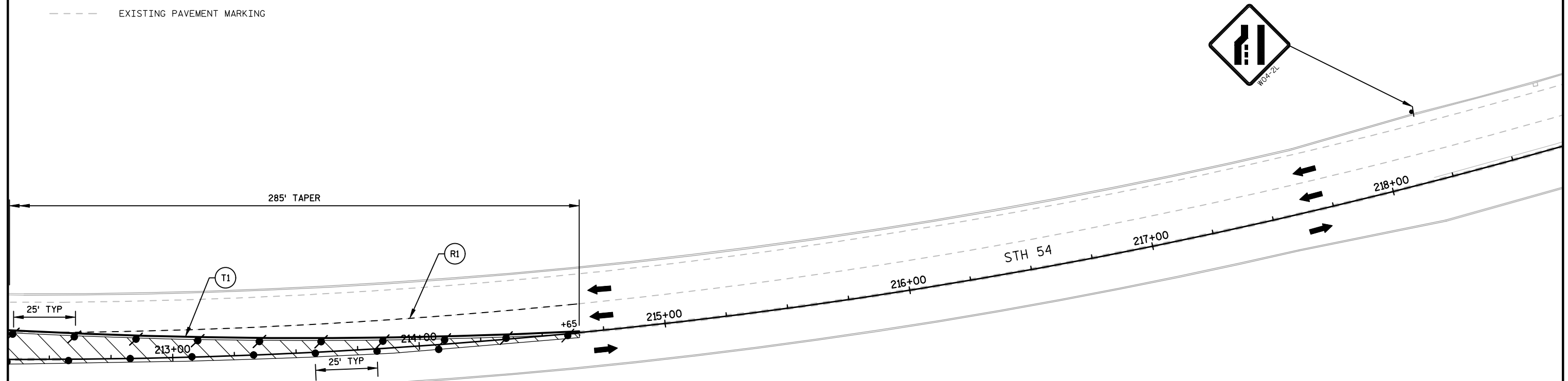
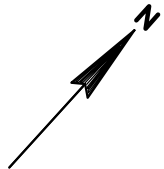
TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)

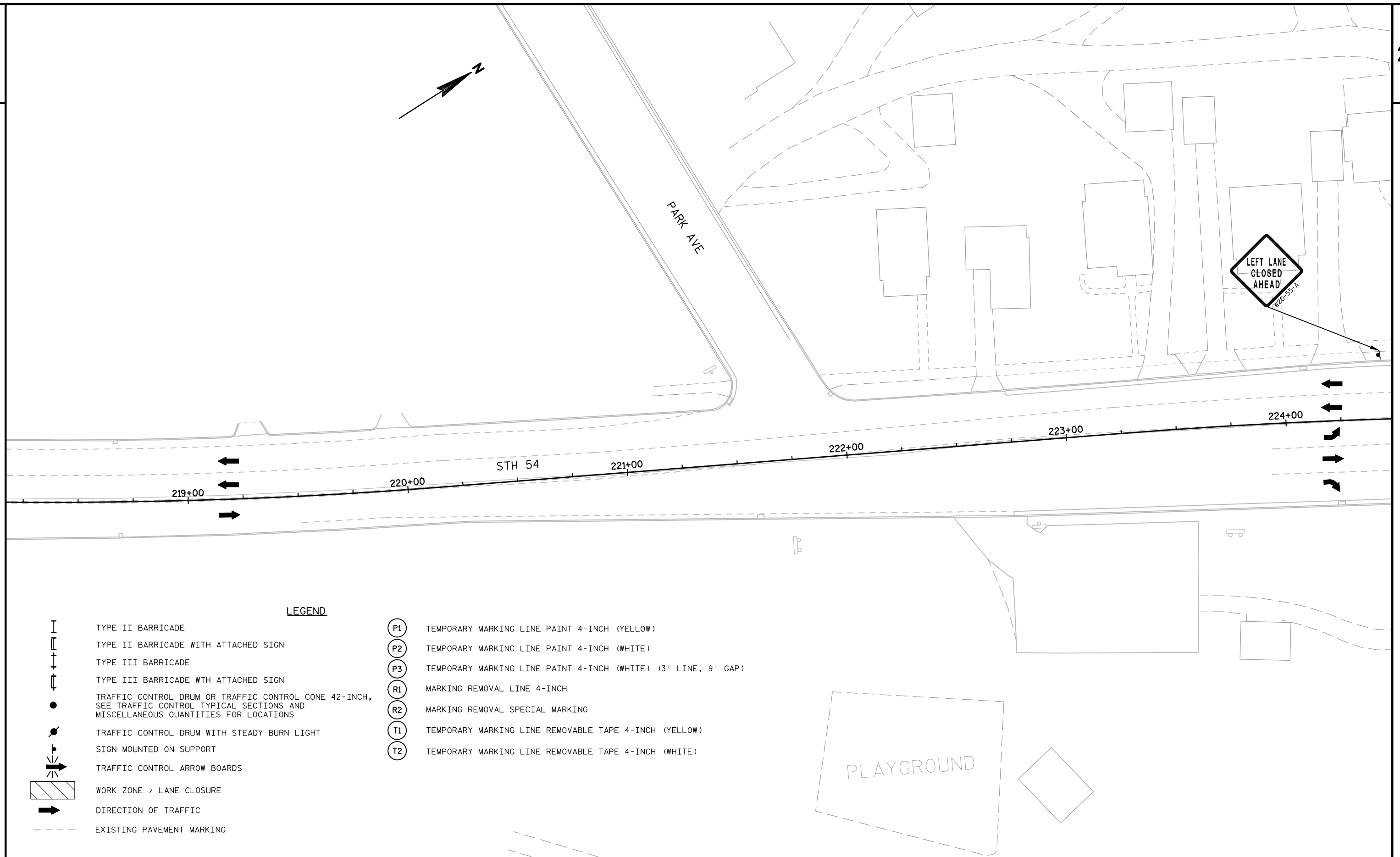
NOTE
FLAGGING TO ONE LANE MAY BE REQUIRED DURING REMOVAL OF
ISLAND; RESTORE ADJACENT SLOPES TO 3:1 OR LESS DURING
NON-WORKING HOURS.



LEGEND

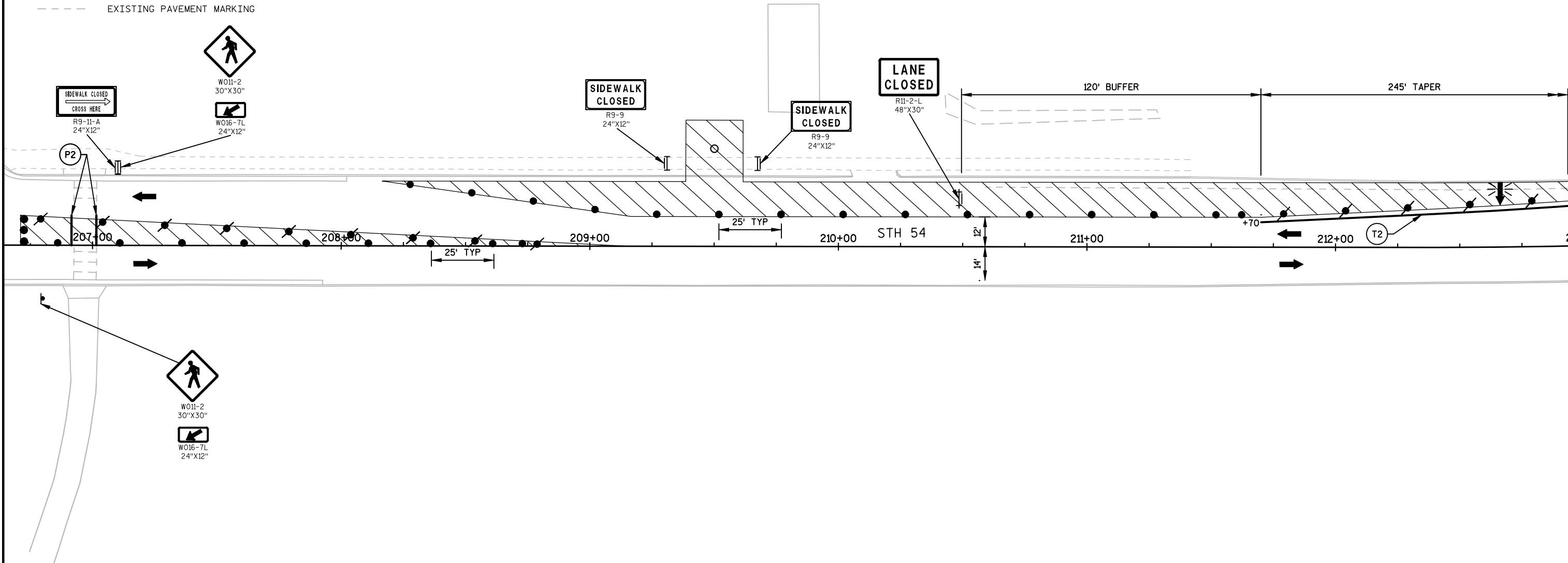
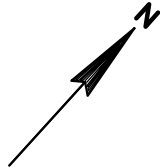
- | | | | |
|---|---|---|---|
|  | TYPE II BARRICADE |  | TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW) |
|  | TYPE II BARRICADE WITH ATTACHED SIGN |  | TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) |
|  | TYPE III BARRICADE |  | TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP) |
|  | TYPE III BARRICADE WITH ATTACHED SIGN |  | MARKING REMOVAL LINE 4-INCH |
|  | TRAFFIC CONTROL DRUM OR TRAFFIC CONTROL CONE 42-INCH, SEE TRAFFIC CONTROL TYPICAL SECTIONS AND MISCELLANEOUS QUANTITIES FOR LOCATIONS |  | MARKING REMOVAL SPECIAL MARKING |
|  | TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT |  | TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW) |
|  | SIGN MOUNTED ON SUPPORT |  | TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE) |
|  | TRAFFIC CONTROL ARROW BOARDS | | |
|  | WORK ZONE / LANE CLOSURE | | |
|  | DIRECTION OF TRAFFIC | | |
| | EXISTING PAVEMENT MARKING | | |

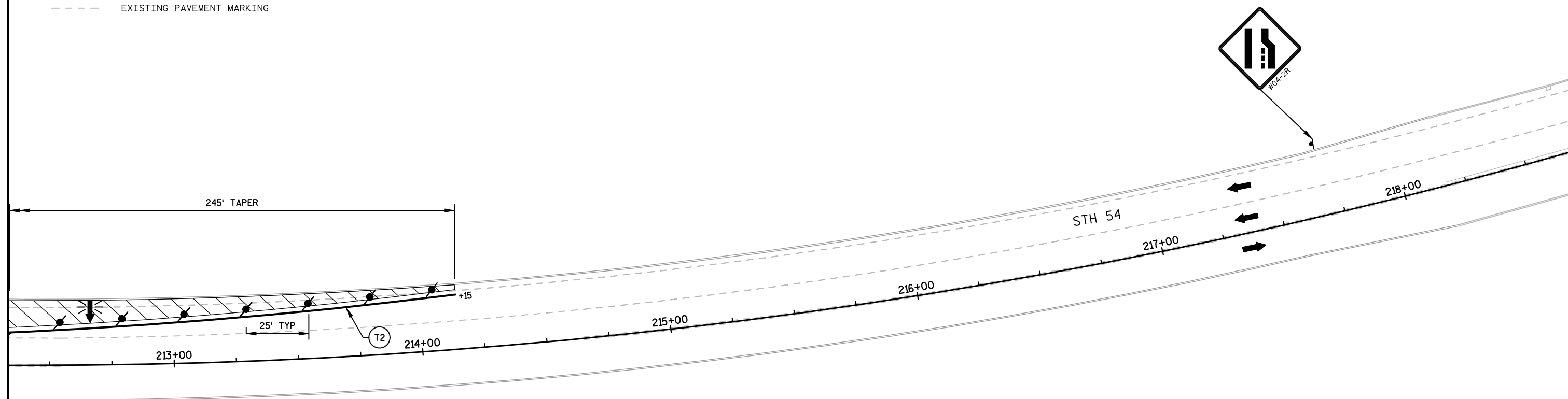
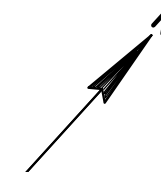
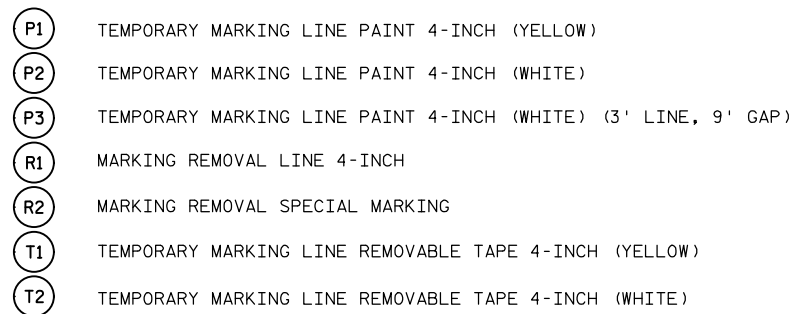
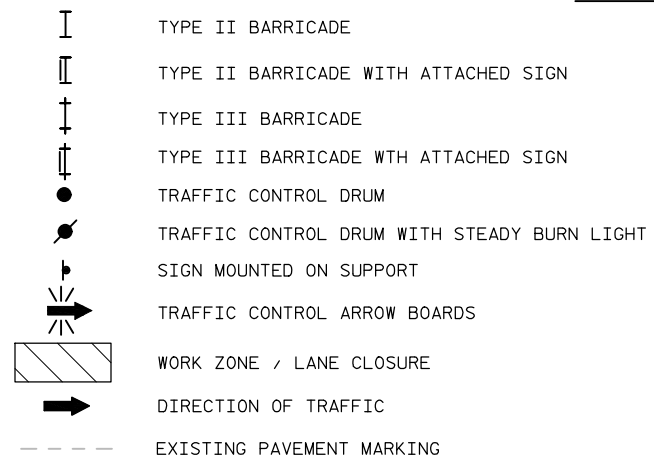


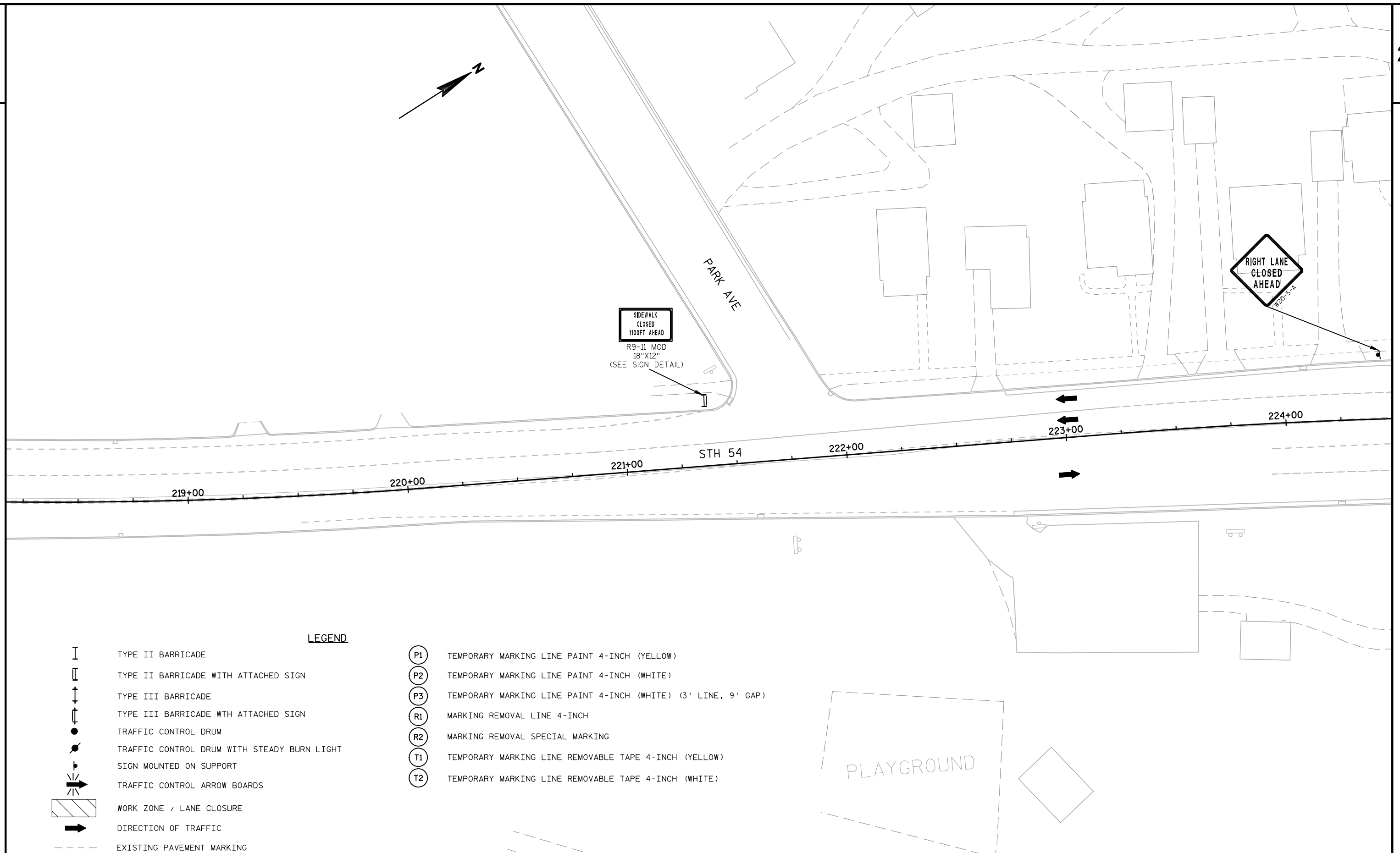


LEGEND

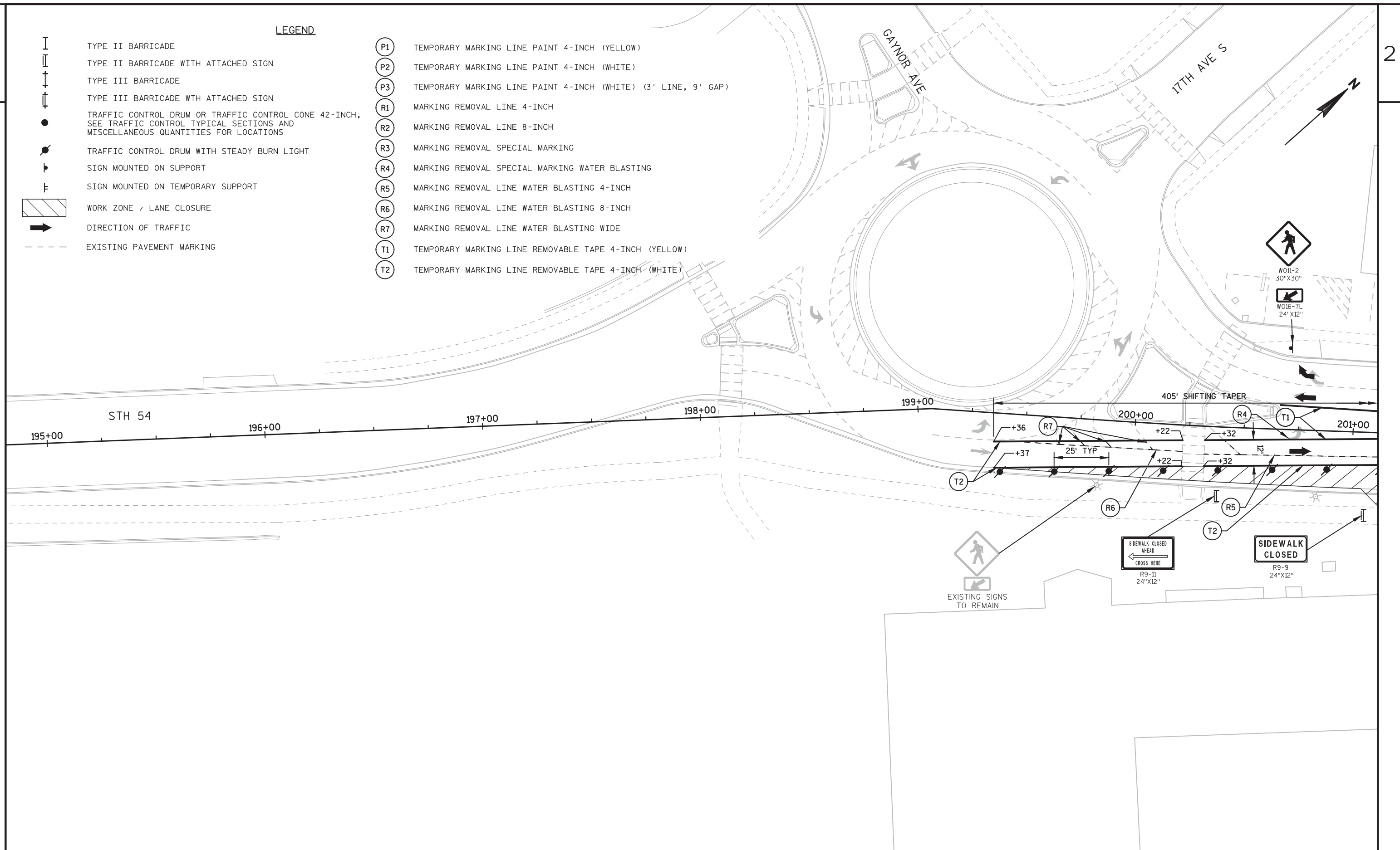
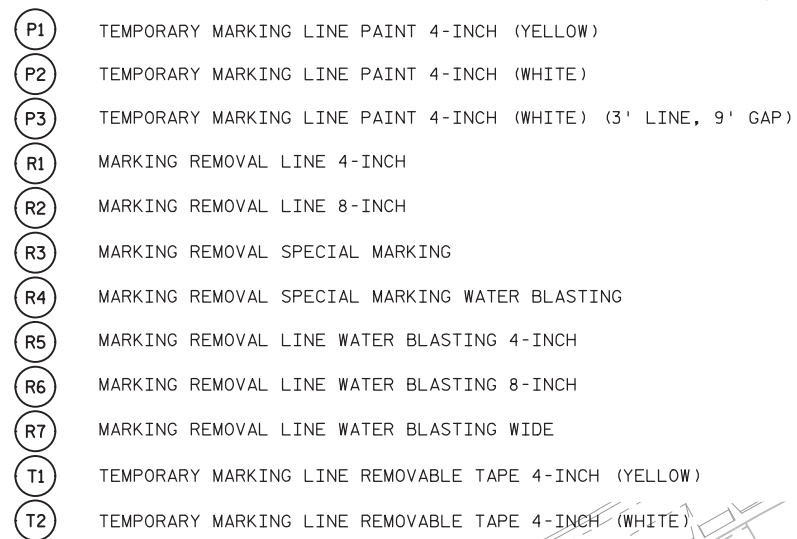
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|--|---|--|---|
| | TYPE II BARRICADE | | TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW) |
| | TYPE II BARRICADE WITH ATTACHED SIGN | | TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) |
| | TYPE III BARRICADE | | TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP) |
| | TYPE III BARRICADE WITH ATTACHED SIGN | | MARKING REMOVAL LINE 4-INCH |
| | TRAFFIC CONTROL DRUM | | MARKING REMOVAL SPECIAL MARKING |
| | TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT | | TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW) |
| | SIGN MOUNTED ON SUPPORT | | TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE) |
| | TRAFFIC CONTROL ARROW BOARDS | | |
| | WORK ZONE / LANE CLOSURE | | |
| | DIRECTION OF TRAFFIC | | |
| | EXISTING PAVEMENT MARKING | | |




























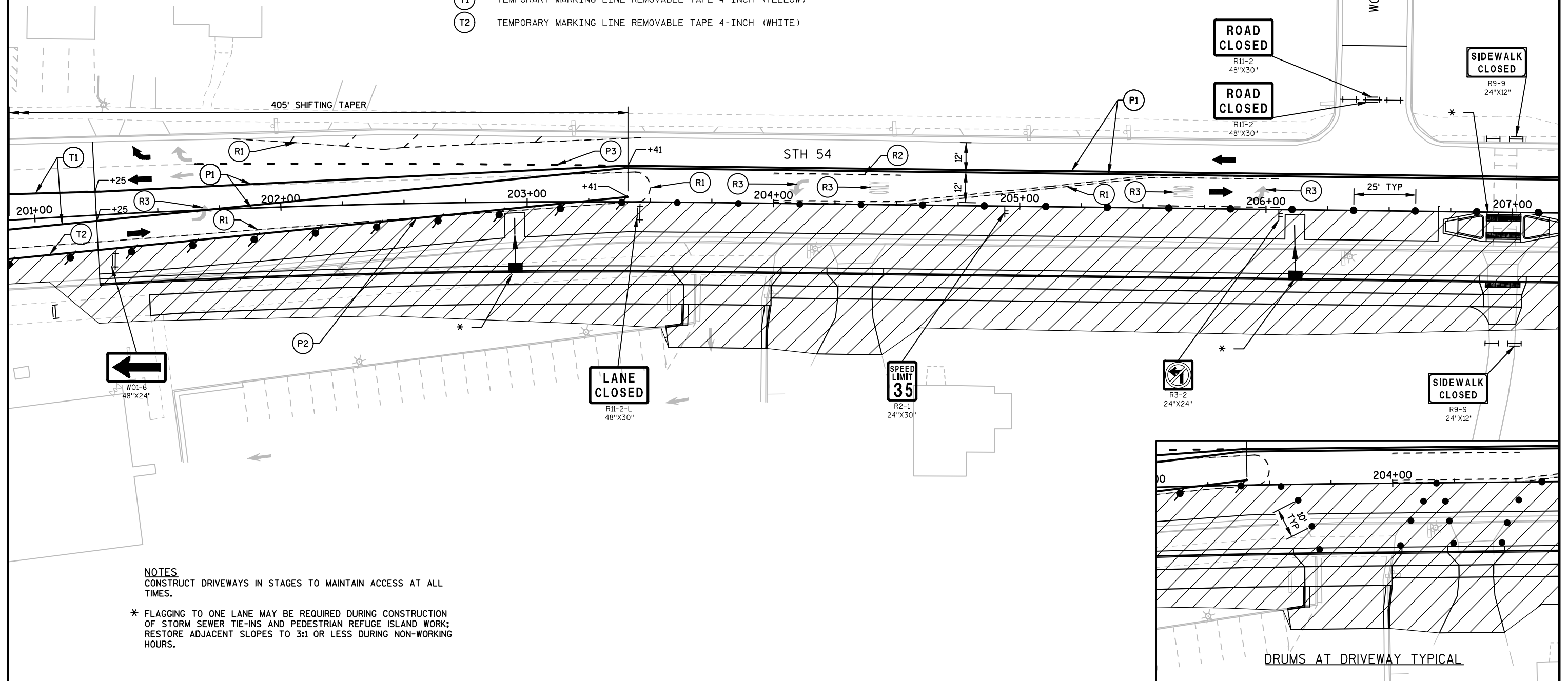
**LEGEND**

	TYPE II BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW)
	TYPE II BARRICADE WITH ATTACHED SIGN		TEMPORARY MARKING LINE PAINT 4-INCH (WHITE)
	TYPE III BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP)
	TYPE III BARRICADE WITH ATTACHED SIGN		MARKING REMOVAL LINE 4-INCH
	TRAFFIC CONTROL DRUM		MARKING REMOVAL SPECIAL MARKING
	TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT		TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW)
	SIGN MOUNTED ON SUPPORT		TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)
	TRAFFIC CONTROL ARROW BOARDS		
	WORK ZONE / LANE CLOSURE		
	DIRECTION OF TRAFFIC		
	EXISTING PAVEMENT MARKING		


























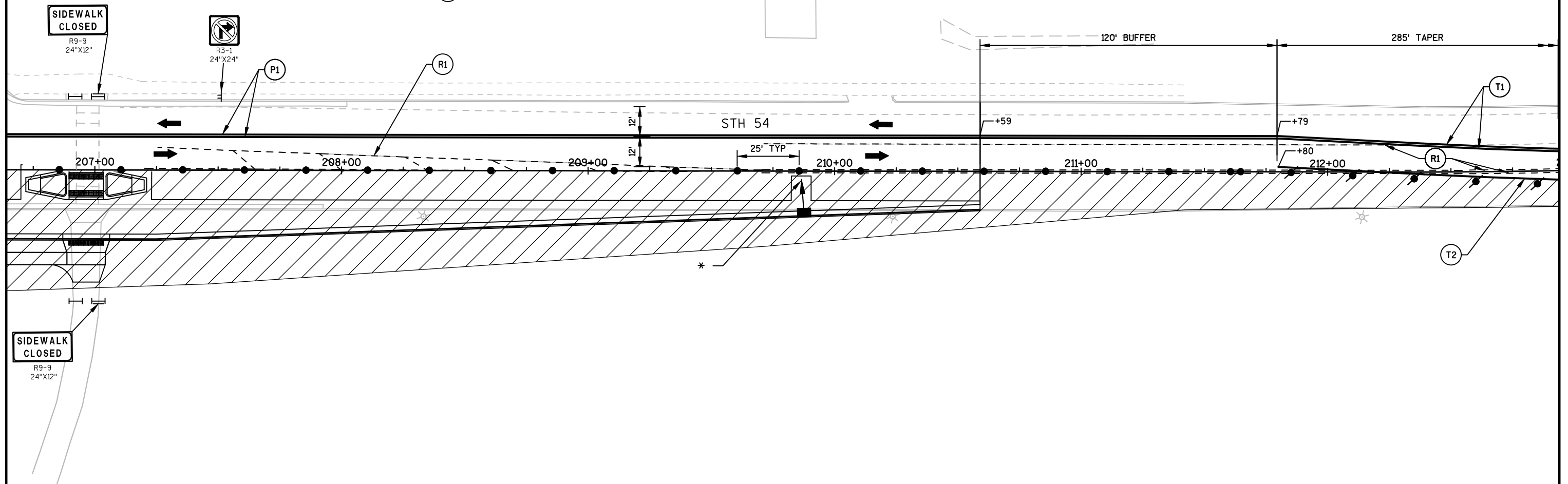
LEGEND

	TYPE II BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW)
	TYPE II BARRICADE WITH ATTACHED SIGN		TEMPORARY MARKING LINE PAINT 4-INCH (WHITE)
	TYPE III BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP)
	TYPE III BARRICADE WITH ATTACHED SIGN		MARKING REMOVAL LINE 4-INCH
	TRAFFIC CONTROL DRUM OR TRAFFIC CONTROL CONE 42-INCH, SEE TRAFFIC CONTROL TYPICAL SECTIONS AND MISCELLANEOUS QUANTITIES FOR LOCATIONS		MARKING REMOVAL LINE 8-INCH
	TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT		MARKING REMOVAL SPECIAL MARKING
	SIGN MOUNTED ON SUPPORT		MARKING REMOVAL SPECIAL MARKING WATER BLASTING
	SIGN MOUNTED ON TEMPORARY SUPPORT		MARKING REMOVAL LINE WATER BLASTING 4-INCH
	WORK ZONE / LANE CLOSURE		MARKING REMOVAL LINE WATER BLASTING 8-INCH
	DIRECTION OF TRAFFIC		MARKING REMOVAL LINE WATER BLASTING WIDE
	EXISTING PAVEMENT MARKING		TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW)
			TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)



LEGEND

	TYPE II BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW)
	TYPE II BARRICADE WITH ATTACHED SIGN		TEMPORARY MARKING LINE PAINT 4-INCH (WHITE)
	TYPE III BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP)
	TYPE III BARRICADE WITH ATTACHED SIGN		MARKING REMOVAL LINE 4-INCH
	TRAFFIC CONTROL DRUM OR TRAFFIC CONTROL CONE 42-INCH, SEE TRAFFIC CONTROL TYPICAL SECTIONS AND MISCELLANEOUS QUANTITIES FOR LOCATIONS		MARKING REMOVAL LINE 8-INCH
	TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT		MARKING REMOVAL SPECIAL MARKING
	SIGN MOUNTED ON SUPPORT		MARKING REMOVAL SPECIAL MARKING WATER BLASTING
	SIGN MOUNTED ON TEMPORARY SUPPORT		MARKING REMOVAL LINE WATER BLASTING 4-INCH
	WORK ZONE / LANE CLOSURE		MARKING REMOVAL LINE WATER BLASTING 8-INCH
	DIRECTION OF TRAFFIC		MARKING REMOVAL LINE WATER BLASTING WIDE
	EXISTING PAVEMENT MARKING		TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW)
			TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)


























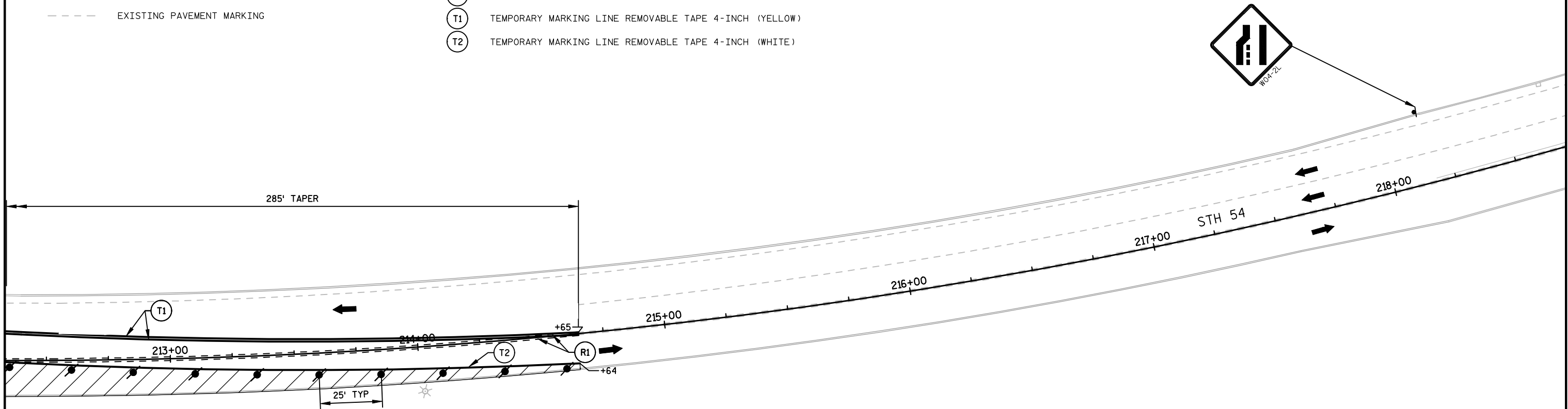
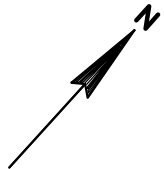
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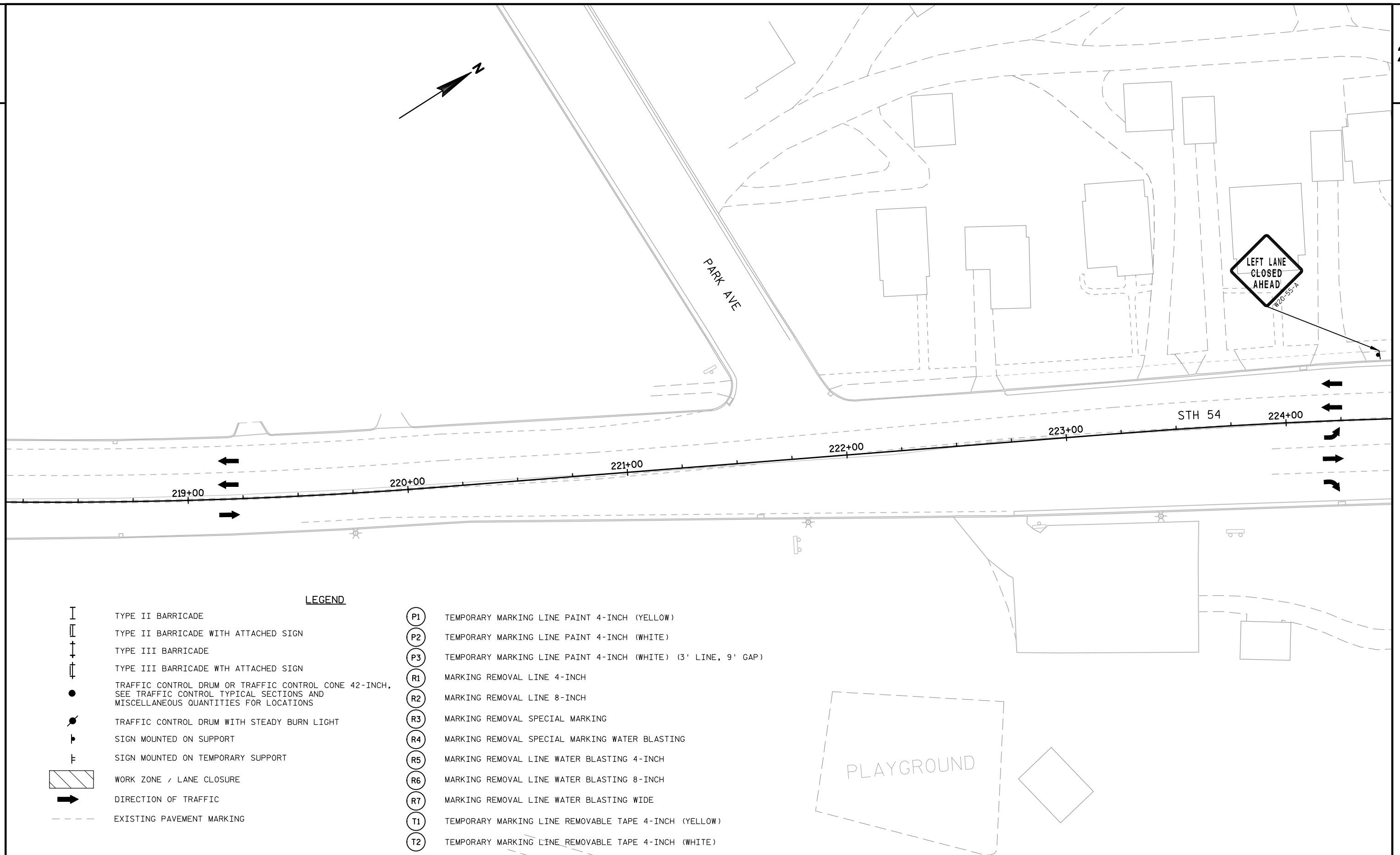
CONSTRUCT DRIVEWAYS IN STAGES TO MAINTAIN ACCESS AT ALL TIMES

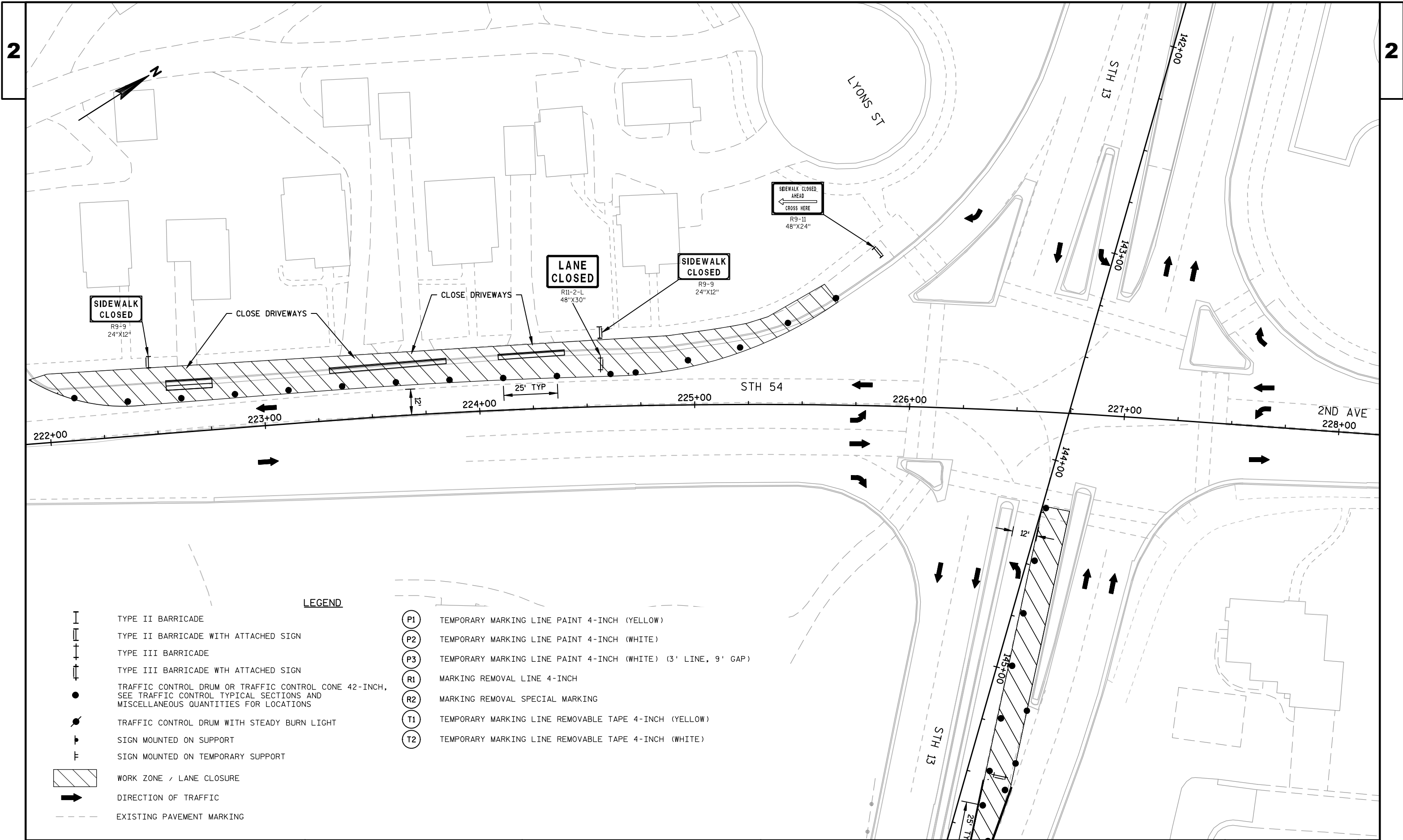
* FLAGGING TO ONE LANE MAY BE REQUIRED DURING CONSTRUCTION OF STORM SEWER TIE-INS AND PEDESTRIAN REFUGE ISLAND WORK; RESTORE SLOPES TO 3:1 OR LESS DURING NON-WORKING HOURS

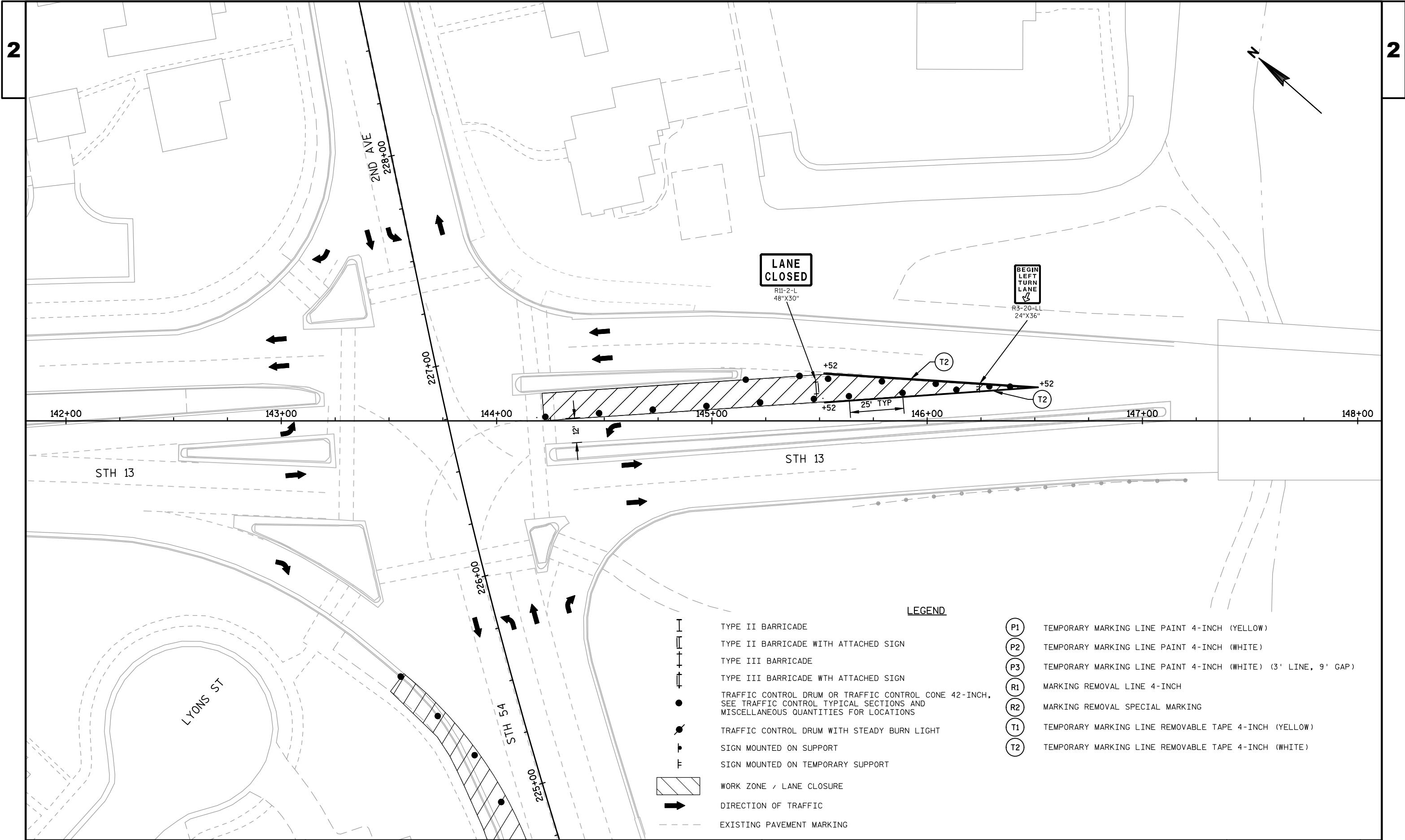
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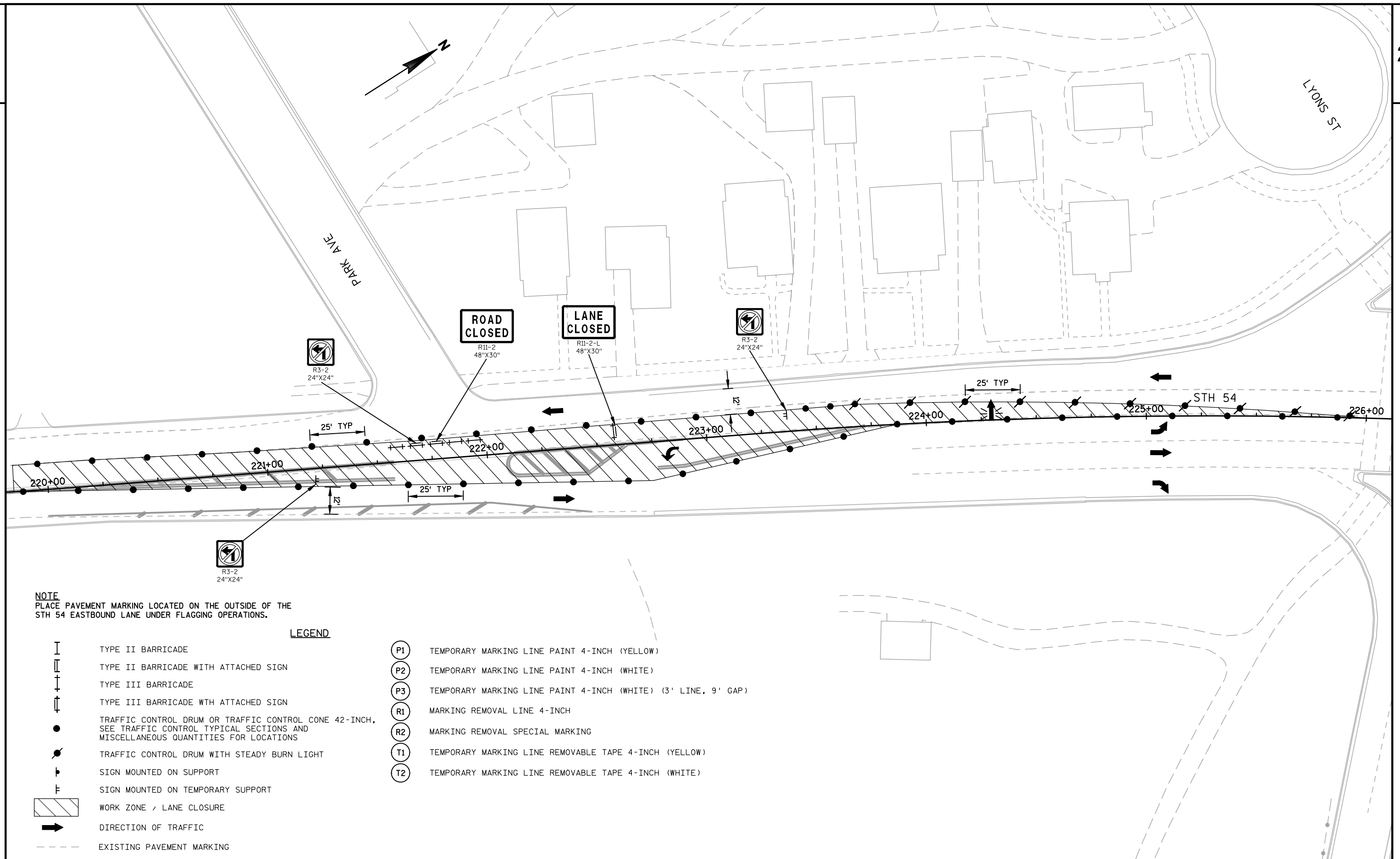
- | | | | |
|---|---|---|---|
|  | TYPE II BARRICADE |  | TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW) |
|  | TYPE II BARRICADE WITH ATTACHED SIGN |  | TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) |
|  | TYPE III BARRICADE |  | TEMPORARY MARKING LINE PAINT 4-INCH (WHITE) (3' LINE, 9' GAP) |
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|  | TRAFFIC CONTROL DRUM WITH STEADY BURN LIGHT |  | MARKING REMOVAL SPECIAL MARKING |
|  | SIGN MOUNTED ON SUPPORT |  | MARKING REMOVAL SPECIAL MARKING WATER BLASTING |
|  | SIGN MOUNTED ON TEMPORARY SUPPORT |  | MARKING REMOVAL LINE WATER BLASTING 4-INCH |
|  | WORK ZONE / LANE CLOSURE |  | MARKING REMOVAL LINE WATER BLASTING 8-INCH |
|  | DIRECTION OF TRAFFIC |  | MARKING REMOVAL LINE WATER BLASTING WIDE |
|  | EXISTING PAVEMENT MARKING |  | TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW) |
| | |  | TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE) |

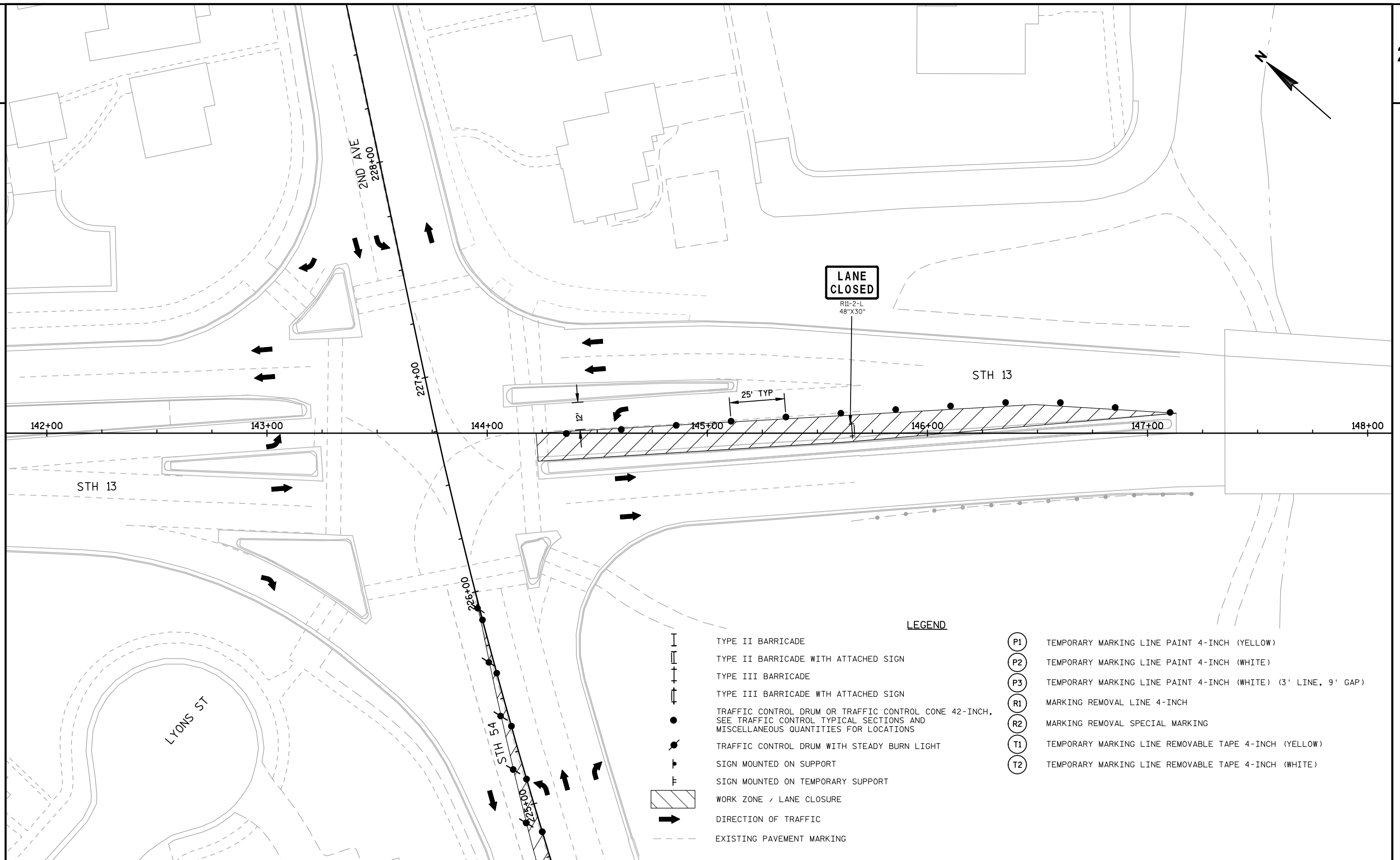












Estimate Of Quantities

6999-03-70					
Line	Item	Item Description	Unit	Total	Qty
0002	201.0120	Clearing	ID	218.000	218.000
0004	201.0220	Grubbing	ID	218.000	218.000
0006	204.0100	Removing Pavement	SY	115.000	115.000
0008	204.0115	Removing Asphaltic Surface Butt Joints	SY	28.000	28.000
0010	204.0120	Removing Asphaltic Surface Milling	SY	4,250.000	4,250.000
0012	204.0130	Removing Curb	LF	12.000	12.000
0014	204.0150	Removing Curb & Gutter	LF	1,200.000	1,200.000
0016	204.0155	Removing Concrete Sidewalk	SY	215.000	215.000
0018	204.0215	Removing Catch Basins	EACH	3.000	3.000
0020	204.0245	Removing Storm Sewer (size) 01. 12-Inch	LF	15.000	15.000
0022	205.0100	Excavation Common	CY	1,333.000	1,333.000
0024	211.0100	Prepare Foundation for Asphaltic Paving (project) 01. 6999-03-70	LS	1.000	1.000
0026	213.0100	Finishing Roadway (project) 01. 6999-03-70	EACH	1.000	1.000
0028	305.0110	Base Aggregate Dense 3/4-Inch	TON	10.000	10.000
0030	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,265.000	1,265.000
0032	416.0260	Concrete Driveway HES 6-Inch	SY	95.000	95.000
0034	416.0610	Drilled Tie Bars	EACH	60.000	60.000
0036	455.0605	Tack Coat	GAL	350.000	350.000
0038	460.2000	Incentive Density HMA Pavement	DOL	660.000	660.000
0040	460.6223	HMA Pavement 3 MT 58-28 S	TON	240.000	240.000
0042	460.6424	HMA Pavement 4 MT 58-28 H	TON	780.000	780.000
0044	465.0110	Asphaltic Surface Patching	TON	45.000	45.000
0046	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	25.000	25.000
0048	520.8000	Concrete Collars for Pipe	EACH	3.000	3.000
0050	601.0110	Concrete Curb Type D	LF	12.000	12.000
0052	601.0407	Concrete Curb & Gutter 18-Inch Type D	LF	90.000	90.000
0054	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	85.000	85.000
0056	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	880.000	880.000
0058	602.0405	Concrete Sidewalk 4-Inch	SF	3,300.000	3,300.000
0060	602.0415	Concrete Sidewalk 6-Inch	SF	125.000	125.000
0062	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	84.000	84.000
0064	608.0412	Storm Sewer Pipe Reinforced Concrete Class IV 12-Inch	LF	53.000	53.000
0066	611.0639	Inlet Covers Type H-S	EACH	3.000	3.000
0068	611.1230	Catch Basins 2x3-FT	EACH	3.000	3.000
0070	611.8110	Adjusting Manhole Covers	EACH	4.000	4.000
0072	618.0100	Maintenance And Repair of Haul Roads (project) 01. 6999-03-70	EACH	1.000	1.000
0074	619.1000	Mobilization	EACH	1.000	1.000

Estimate Of Quantities

6999-03-70

Line	Item	Item Description	Unit	Total	Qty
0076	620.0300	Concrete Median Sloped Nose	SF	80.000	80.000
0078	624.0100	Water	MGAL	12.000	12.000
0080	625.0100	Topsoil	SY	1,200.000	1,200.000
0082	628.1504	Silt Fence	LF	480.000	480.000
0084	628.1520	Silt Fence Maintenance	LF	480.000	480.000
0086	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0088	628.1910	Mobilizations Emergency Erosion Control	EACH	1.000	1.000
0090	628.2006	Erosion Mat Urban Class I Type A	SY	1,200.000	1,200.000
0092	628.7005	Inlet Protection Type A	EACH	4.000	4.000
0094	628.7015	Inlet Protection Type C	EACH	15.000	15.000
0096	629.0210	Fertilizer Type B	CWT	1.000	1.000
0098	630.0140	Seeding Mixture No. 40	LB	25.000	25.000
0100	631.0300	Sod Water	MGAL	20.000	20.000
0102	634.0811	Posts Tubular Steel 2x2-Inch X 11-FT	EACH	6.000	6.000
0104	634.0814	Posts Tubular Steel 2x2-Inch X 14-FT	EACH	3.000	3.000
0106	637.1220	Signs Type I Reflective SH	SF	83.000	83.000
0108	637.2210	Signs Type II Reflective H	SF	56.180	56.180
0110	637.2230	Signs Type II Reflective F	SF	23.750	23.750
0112	638.2602	Removing Signs Type II	EACH	15.000	15.000
0114	638.3000	Removing Small Sign Supports	EACH	11.000	11.000
0116	641.8100	Overhead Sign Support (structure) 01. S-71-0033	LS	1.000	1.000
0118	643.0300	Traffic Control Drums	DAY	1,736.000	1,736.000
0120	643.0310.S	Temporary Portable Rumble Strips	LS	1.000	1.000
0122	643.0410	Traffic Control Barricades Type II	DAY	268.000	268.000
0124	643.0420	Traffic Control Barricades Type III	DAY	217.000	217.000
0126	643.0705	Traffic Control Warning Lights Type A	DAY	350.000	350.000
0128	643.0715	Traffic Control Warning Lights Type C	DAY	1,117.000	1,117.000
0130	643.0800	Traffic Control Arrow Boards	DAY	13.000	13.000
0132	643.0900	Traffic Control Signs	DAY	3,427.000	3,427.000
0134	643.1050	Traffic Control Signs PCMS	DAY	27.000	27.000
0136	643.1070	Traffic Control Cones 42-Inch	DAY	1,587.000	1,587.000
0138	643.5000	Traffic Control	EACH	1.000	1.000
0140	646.1020	Marking Line Epoxy 4-Inch	LF	1,500.000	1,500.000
0142	646.3020	Marking Line Epoxy 8-Inch	LF	360.000	360.000
0144	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	5,450.000	5,450.000
0146	646.5020	Marking Arrow Epoxy	EACH	9.000	9.000
0148	646.5120	Marking Word Epoxy	EACH	2.000	2.000
0150	646.7120	Marking Diagonal Epoxy 12-Inch	LF	300.000	300.000
0152	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	180.000	180.000
0154	646.7520	Marking Crosswalk Epoxy Ladder Pattern 24-Inch	LF	200.000	200.000

Estimate Of Quantities

6999-03-70

Line	Item	Item Description	Unit	Total	Qty
0156	646.8120	Marking Curb Epoxy	LF	55.000	55.000
0158	646.8220	Marking Island Nose Epoxy	EACH	2.000	2.000
0160	646.9000	Marking Removal Line 4-Inch	LF	2,535.000	2,535.000
0162	646.9010	Marking Removal Line Water Blasting 4-Inch	LF	1,320.000	1,320.000
0164	646.9100	Marking Removal Line 8-Inch	LF	160.000	160.000
0166	646.9110	Marking Removal Line Water Blasting 8-Inch	LF	155.000	155.000
0168	646.9200	Marking Removal Line Wide	LF	25.000	25.000
0170	646.9210	Marking Removal Line Water Blasting Wide	LF	335.000	335.000
0172	646.9300	Marking Removal Special Marking	EACH	5.000	5.000
0174	646.9310	Marking Removal Special Marking Water Blasting	EACH	5.000	5.000
0176	649.0105	Temporary Marking Line Paint 4-Inch	LF	2,160.000	2,160.000
0178	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	2,205.000	2,205.000
0180	650.4000	Construction Staking Storm Sewer	EACH	3.000	3.000
0182	650.5000	Construction Staking Base	LF	934.000	934.000
0184	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,060.000	1,060.000
0186	650.8000	Construction Staking Resurfacing Reference	LF	2,315.000	2,315.000
0188	650.8500	Construction Staking Electrical Installations (project) 01. 6999-03-70	LS	1.000	1.000
0190	650.9910	Construction Staking Supplemental Control (project) 01. 6999-03-70	LS	1.000	1.000
0192	650.9920	Construction Staking Slope Stakes	LF	934.000	934.000
0194	652.0325	Conduit Rigid Nonmetallic Schedule 80 2-Inch	LF	1,135.000	1,135.000
0196	655.0610	Electrical Wire Lighting 12 AWG	LF	1,350.000	1,350.000
0198	655.0630	Electrical Wire Lighting 4 AWG	LF	8,365.000	8,365.000
0200	690.0150	Sawing Asphalt	LF	1,650.000	1,650.000
0202	690.0250	Sawing Concrete	LF	125.000	125.000
0204	SPV.0060	Special 01. Adjusting Water Valve Boxes	EACH	6.000	6.000
0206	SPV.0060	Special 02. Adjusting Sanitary Manhole Cover	EACH	3.000	3.000
0208	SPV.0060	Special 03. Reconstructing Sanitary Manhole	EACH	1.000	1.000
0210	SPV.0060	Special 04. Remove and Reinstall Street Light Assembly	EACH	5.000	5.000
0212	SPV.0060	Special 05. Research and Locate Existing Land Parcel Monuments	EACH	2.000	2.000
0214	SPV.0060	Special 06. Verify and Replace Existing Land Parcel Monuments	EACH	2.000	2.000
0216	SPV.0090	Special 01. Concrete Curb & Gutter HES 30-Inch Type D	LF	100.000	100.000
0218	SPV.0165	Special 01. Salvage and Replace Brick Pavers	SF	32.000	32.000
0220	SPV.0165	Special 02. Salvage Brick Pavers	SF	160.000	160.000
0222	SPV.0180	Special 01. Preparing Topsoil for Lawn Type Turf	SY	1,200.000	1,200.000

Estimate Of Quantities

6999-03-70

3

CLEARING AND GRUBBING ITEMS

CATEGORY	STAGE	STATION	OFFSET	201.0120	201.0220
				CLEARING ID	GRUBBING ID
0010					
	3	202+40	RT	32	32
	3	204+48	RT	30	30
	3	204+55	RT	15	15
	3	204+88	RT	18	18
	3	205+02	RT	36	36
	3	205+69	RT	36	36
	3	206+08	RT	15	15
	3	206+34	RT	36	36
TOTALS				218	218

3

REMOVAL ITEMS

CATEGORY	STAGE	STATION	TO	STATION	OFFSET	204.0100	204.0115	204.0120	204.0130	204.0150	204.0155	SPV.0165.02
						REMOVING PAVEMENT SY	REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	REMOVING ASPHALTIC SURFACE MILLING SY	REMOVING CURB LF	REMOVING CURB & GUTTER LF	REMOVING CONCRETE SIDEWALK SY	SALVAGE BRICK PAVERS SF
0010												
	1	206+73	-	207+22	LT & RT	8	-	--	--	115	15	160
	3	201+25	-	210+59	RT	--	-	--	--	932	-	--
	3	201+45	-	203+67	RT	--	-	--	--	-	200	--
	3	202+24	-	210+25	RT	--	-	--	--	-	-	--
	3	203+50	-	204+00	RT	50	-	--	12	46	-	--
	3	204+23	-	204+46	RT	16	-	--	--	-	-	--
	4 & 5	201+25	-	210+59	LT & RT	--	28	4,250	--	-	-	--
	6	222+55	-	222+77	LT	9	-	--	--	22	-	--
	6	223+32	-	223+86	LT	22	-	--	--	54	-	--
	6	224+10	-	224+40	LT	10	-	--	--	31	-	--
TOTALS						115	28	4,250	12	1,200	215	160

EARTHWORK SUMMARY

DIVISION	LOCATION	ITEM #205.0100 EXCAVATION COMMON (NOTE 1) (CY)	SALVAGED / UNUSEABLE PAVEMENT MATERIAL (NOTE 2)	AVAILABLE MATERIAL (NOTE 3)	UNEXPANDED FILL	EXPANDED FILL (NOTE 4)	MASS ORDINATE +/- (NOTE 5)
						FACTOR 1.25	
STH 54							
1	STA 201+25 - STA 210+59	1,333	150	1,183	15	19	1,164
TOTALS		1,333	150	1,183	15	19	1,164

- 1) SALVAGED/UNSUALE PAVEMENT MATERIAL IS INCLUDED IN CUT
- 2) SALVAGED/UNUSABLE PAVEMENT MATERIAL = (AREA OF PROJECT PAVEMENT, CURB & GUTTER, AND SIDEWALK REMOVAL) * (TYPICAL DEPTH)
- 3) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUALE PAVEMENT MATERIAL
- 4) EXPANDED FILL FACTOR = (UNEXPANDED FILL) * (FILL FACTOR)
- 5) MASS ORDINATE = AVAILABLE MATERIAL - (EXPANDED FILL); POSITIVE INDICATES AN EXCESS OF MATERIAL

3

REMOVING DRAINAGE ITEMS

		204.0215			204.0245	
		REMOVING CATCH BASINS			REMOVING STORM SEWER (12-INCH)	
CATEGORY	STAGE	STATION	TO	OFFSET	EACH	LF
0010						
	3	202+95	-	RT	1	5
	3	206+12	-	RT	1	5
	3	209+87	-	RT	1	5
TOTALS					3	15

PREPARE FOUNDATION ITEMS

		211.0100	
		PREPARE FOUNDATION FOR ASPHALTIC PAVING (6999-03-70)	
CATEGORY	LOCATION	LS	
0010	PROJECT	1	
TOTAL		1	

BASE AGGREGATE ITEMS

		305.0110			305.0120		
		BASE AGGREGATE DENSE 3/4-INCH			BASE AGGREGATE DENSE 1 1/4-INCH		
CATEGORY	STAGE	STATION	TO	OFFSET	TON	TON	COMMENTS
0010							
	3	201+25	-	210+59 RT	--	1,150	
	3	203+64	-	207+05 RT	--	65	DRIVEWAYS
	6	222+55	-	224+40 LT	10	--	
UNDISTRIBUTED					--	50	ADJUSTING MANHOLES AND TEMPORARY WEDGE
TOTALS					10	1,265	

3

DRIVEWAY SURFACE ITEMS

		416.0260			465.0120		
		CONCRETE DRIVEWAY HES 6-INCH			ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES		
CATEGORY	STAGE	STATION	TO	OFFSET	SY	TON	
0010							
	3	203+64	-	204+04 RT	57	12	
	3	204+22	-	204+45 RT	19	10	
	3	206+81	-	207+05 RT	19	3	
TOTALS					95	25	

DRILLED TIE BARS

					416.0610	
					DRILLED TIE BARS	
CATEGORY	STAGE	STATION	TO	OFFSET	EACH	
0010						
	3	201+25	-	210+59 RT	10	
	6	223+31	-	224+40 LT	50	
TOTAL					60	

ASPHALTIC ITEMS

		455.0605			460.6223		460.6424		465.0110	
		TACK COAT			HMA PAVEMENT 3 MT 58-28 S		HMA PAVEMENT 4 MT 58-28 H		ASPHALTIC SURFACE PATCHING	
CATEGORY	STAGE	STATION	TO	OFFSET	GAL	TON	TON	TON	COMMENTS	
0010										
	1	206+72	-	207+21 LT	--	--	--	23		
	3, 4 & 5	201+25	-	210+59 LT & RT	350	240	780	--		
	4 & 5	201+25	-	210+59 LT & RT	--	--	--	20	ADJUSTING MANHOLES	
6		222+55	-	222+77 LT	--	--	--	2		
TOTALS					350	240	780	45		

CONCRETE CURB & CONCRETE CURB & GUTTER ITEMS

		601.0110			601.0407		601.0409		601.0411		SPV.0090.01	
		CONCRETE CURB TYPE D			CONCRETE CURB & GUTTER 18-INCH TYPE D		CONCRETE CURB & GUTTER 30-INCH TYPE A		CONCRETE CURB & GUTTER 30-INCH TYPE D		CONCRETE CURB & GUTTER HES 30-INCH TYPE D	
CATEGORY	STAGE	STATION	TO	OFFSET	LF	LF	LF	LF	LF	LF		
0010												
	3	201+25	-	210+59 RT	--	--	--	834	100			
	3	203+50	-	204+00 RT	12	--	--	24	--			
	3	206+72	-	207+21 RT	--	90	--	--	--			
	6	222+55	-	222+77 LT	--	--	--	22	--			
6		223+31	-	224+40 LT	--	--	85	--	--			
TOTALS					12	90	85	880	100			

PROJECT NO:6999-03-70

HWY:STH 54

COUNTY:WOOD

MISCELLANEOUS QUANTITIES

SHEET

E

3

CONCRETE SIDEWALK ITEMS

						602.0405	602.0415	602.0505
						CONCRETE SIDEWALK 4-INCH SF	CONCRETE SIDEWALK 6-INCH SF	CURB RAMP DETECTABLE WARNING FIELD YELLOW SF
CATEGORY	STAGE	STATION	TO	STATION	OFFSET			
0010	3	201+45	-	204+00	RT	1,780	--	--
	3	206+72	-	207+21	RT	140	125	84
CATEGORY TOTALS						1,920	125	84
0020	3	204+00	-	206+88	RT	1,380	--	--
CATEGORY TOTALS						1,380	--	--
TOTALS						3,300	125	84

3

CONCRETE MEDIAN NOSE

						620.0300	
						CONCRETE MEDIAN SLOPED NOSE SF	COMMENTS
CATEGORY	STAGE	STATION	TO	STATION	OFFSET		
0010	3	206+72	-	207+21	RT	80	TYPE 1
TOTAL						80	

STORM SEWER STRUCTURES

						520.8000	608.0412	611.0639	611.1230
						CONCRETE COLLARS FOR PIPE EACH	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 12-INCH LF	INLET COVERS TYPE H-S EACH	CATCH BASINS 2X3-FT EACH
CATEGORY	STAGE	STATION	OFFSET	STRUCTURE NUMBER					
0010	3	202+95.00	27.5' RT	1		1	21	1	1
	3	206+12.27	27.5' RT	2		1	21	1	1
	3	209+87.73	17.65' RT	3		1	11	1	1
TOTALS						3	53	3	3

ADJUSTING & RECONSTRUCTING ITEMS

						611.8110	SPV.0060.01	SPV.0060.02	SPV.0060.03
						ADJUSTING MANHOLE COVERS EACH	ADJUSTING WATER VALVE BOXES EACH	ADJUSTING SANITARY MANHOLE COVER EACH	RECONSTRUCTING SANITARY MANHOLE EACH
CATEGORY	STAGE	STATION	TO	STATION	OFFSET				
0010	4 & 5	201+25	-	210+59	LT & RT	4	-	-	-
CATAGORY TOTALS						4	-	-	-
0030	4 & 5	201+25	-	210+59	LT & RT	-	6	3	1
CATEGORY TOTALS						-	6	3	1
TOTALS						4	6	3	1

EROSION CONTROL ITEMS

						628.1504	628.1520	628.2006	628.7005	628.7015
						SILT FENCE	SILT FENCE	EROSION MAT	INLET	INLET
						LF	MAINTENANCE	URBAN CLASS I	PROTECTION	PROTECTION
CATEGORY	STAGE	STATION	TO	STATION	OFFSET	LF	LF	TYPE A	TYPE A	TYPE C
0010								SY	EACH	EACH
	3, 4 & 5	200+50	-	210+59	LT & RT	--	--	900	3	9
	3	207+00	-	210+59	RT	380	380	--	--	--
	6 & 7	221+90	-	224+40	LT	--	--	60	--	3
UNDISTRIBUTED						100	100	240	1	3
TOTALS						480	480	1,200	4	15

EROSION CONTROL MOBILIZATION ITEMS

		628.1905	628.1910
		MOBILIZATIONS	MOBILIZATIONS
		EROSION	EMERGENCY
		CONTROL	EROSION
CATEGORY	LOCATION	EACH	EACH
0010	PROJECT	4	1
TOTALS		4	1

TYPE II SIGNS AND SUPPORTS

			634.0811			634.0814		637.2210		637.2230		638.2602		638.3000		SPV.0165.01	
			SIGN DIMENSION W X H			POSTS TUBULAR STEEL 2X2-INCH X 11-FT		POSTS TUBULAR STEEL 2X2-INCH X 14-FT		SIGNS TYPE II REFLECTIVE H		SIGNS TYPE II REFLECTIVE F		REMOVING SMALL SIGN SUPPORTS		SALVAGE AND REPLACE BRICK PAVERS	
CATEGORY	SIGN NUMBER	SIGN CODE	IN	X	IN	EACH	EACH	SF	SF	EACH	EACH	SF	COMMENTS				
0010	1-1	D2-1	54	X	24	2	--	9.00	--	--	--	8					
	1-2	--	--	X	--	--	--	--	--	1	1	--					
	2-1	R3-8-D	36	X	30	1	--	7.50	--	1	1	4					
	2-2	W3-2	36	X	36	--	1	--	9.00	--	--	4					
	2-3	--	--	X	--	--	--	--	--	1	1	4					
	2-4	R2-1	24	X	30	--	--	5.00	--	--	--	--	MOUNT ON LIGHT POLE				
	2-5	--	--	X	--	--	--	--	--	1	--	--					
	2-6	--	--	X	--	--	--	--	--	1	1	--					
	2-7	W2-6	30	X	30	--	1	--	6.25	--	--	4					
	2-8	W13-1	18	X	18	--	--	--	2.25	--	--	--	MOUNT ON SAME POST AS SIGN NO. 2-7				
	2-9	--	--	X	--	--	--	--	--	1	1	--					
	2-10	W8-73	30	X	30	--	1	--	6.25	--	--	4					
	2-11	JV	24	X	72	--	--	12.00	--	--	--	--	MOUNT ON LIGHT POLE				
	2-12	--	--	X	--	--	--	--	--	1	--	--					
2-14	R1-1	30	X	30	1	--	5.18	--	1	1	--						
2-15	--	--	X	--	--	--	--	--	1	1	--						
3-1	R4-7	24	X	30	1	--	5.00	--	--	--	--						
3-2	R4-7	24	X	30	1	--	5.00	X	--	--	--						
3-3	--	--	X	--	--	--	--	--	--	1	1	--					
3-4	--	--	X	--	--	--	--	--	--	1	--	--	MOUNTED ON SAME POST AS NO. 3-3				
3-5	--	--	X	--	--	--	--	--	--	1	1	--					
3-9	--	--	X	--	--	--	--	--	--	1	1	4					
3-10	--	--	X	--	--	--	--	--	--	1	1	--					
CATEGORY TOTALS						6	3	48.68	23.75	14	11	32					
0030	2-13	--	--	X	--	--	--	--	--	1	--	--					
	3-6	JH-1	24	X	45	--	--	7.50	--	--	--	--	MOUNT ON LIGHT POLE				
CATEGORY TOTALS						--	--	7.50	--	1	--	--					
TOTALS						6	3	56.18	23.75	15	11	32					

TYPE I SIGNS

								637.1220
								SIGN
								DIMENSION
								W X H
								SF
CATEGORY	SIGN NUMBER	SIGN CODE	IN	X	IN			COMMENTS
0010	3-7	E6-51	96	X	72		48.00	S-17-0033
	3-8	E6-51	84	X	60		35.00	S-17-0033
	TOTAL							83.00

SIGN STRUCTURES

				641.8100
				OVERHEAD
				SIGN SUPPORT
				(S-71-0033)
CATEGORY	STATION	OFFSET		LS
0010	209+50	LT & RT		1
TOTAL				1

3

RESTORATION ITEMS

625.0100 629.0210 630.0140 SPV.0180.01									
CATEGORY	STAGE	STATION	TO	STATION	OFFSET	TOPSOIL SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 40 LB	PREPARING TOPSOIL FOR LAWN TYPE TURF SY
0010									
	3	200+50	-	210+59	LT & RT	900	0.6	16	900
	6	221+90	-	224+40	LT	60	0.1	2	60
UNDISTRIBUTED						240	0.3	7	240
TOTALS						1,200	1.0	25	1,200

WATER ITEMS

CATEGORY	STAGE	LOCATION	624.0100 WATER MGAL	631.0300 SOD WATER MGAL
0010				
	3	PROJECT - BASE COMPACTION	12	-
	3 & 6	PROJECT - SEEDED AREAS	-	20
TOTALS			12	20

3

TRAFFIC CONTROL ITEMS

		643.0300		643.0410		643.0420		643.0705		643.0715		643.0800		643.0900		643.1050		643.1070		
CATEGORY	STAGE	STAGE DURATION	TRAFFIC CONTROL DRUMS		TRAFFIC CONTROL BARRICADES TYPE II		TRAFFIC CONTROL BARRICADES TYPE III		TRAFFIC CONTROL WARNING LIGHTS TYPE A		TRAFFIC CONTROL WARNING LIGHTS TYPE C		TRAFFIC CONTROL ARROW BOARDS		TRAFFIC CONTROL SIGNS		TRAFFIC CONTROL SIGNS PCMS		TRAFFIC CONTROL CONES 42-INCH	
		DAYS	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY
0010	1	7	12	84	5	35	1	7	2	14	12	84	1	7	52	364	3	21	54	378
SUBTOTALS			84		35		7		14		84		7		364		21		378	
0010	2	5	46	230	4	20	1	5	2	10	19	95	1	5	57	285	--	--	--	--
SUBTOTALS			230		20		5		10		95		5		285		--		--	
0010	3	32	29	928	6	192	5	160	8	256	29	928	--	--	63	2,016	--	--	33	1,056
SUBTOTALS			928		192		160		256		928		--		2,016		--		1,056	
0010	4	4	45	180	--	--	3	12	4	16	--	--	--	--	49	196	--	--	--	--
SUBTOTALS			180		--		12		16		--		--		196		--		--	
0010	5	4	45	180	--	--	3	12	4	16	--	--	--	--	49	196	--	--	--	--
SUBTOTALS			180		--		12		16		--		--		196		--		--	
0010	6	7	16	112	3	21	2	14	4	28	--	--	--	--	52	364	3	6	16	112
SUBTOTALS			112		21		14		28		--		--		364		6		112	
0010	7	1	22	22	--	--	7	7	10	10	10	10	1	1	6	6	--	--	41	41
SUBTOTALS			22		--		7		10		10		1		6		--		41	
TOTALS			1,736		268		217		350		1,117		13		3,427		27		1,587	

TRAFFIC CONTROL

643.5000 TRAFFIC CONTROL EACH		
CATEGORY	LOCATION	
0010	PROJECT	1
TOTAL		1

TEMPORARY PORTABLE RUMBLE STRIPS

643.0310.S TEMPORARY PORTABLE RUMBLE STRIPS LS		
CATEGORY	LOCATION	
0010	PROJECT	1
TOTAL		1

TEMPORARY PAVEMENT MARKING ITEMS														
CATEGORY	STAGE	STATION	TO	STATION	OFFSET	646.9000	646.9100	646.9200	646.9300	649.0105			649.0150	
						MARKING REMOVAL LINE 4-INCH LF	MARKING REMOVAL LINE 8-INCH LF	MARKING REMOVAL LINE WIDE LF	MARKING REMOVAL SPECIAL MARKING EACH	TEMPORARY MARKING LINE PAINT 4-INCH			TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH	
										3' LINE WHITE LF	9' SKIP YELLOW LF	37.5' SKIP WHITE LF	YELLOW LF	WHITE LF
0010	1	211+84	-	214+65	LT	55	--	--	--	--	--	--	280	--
SUBTOTALS						55	--	--	--	--	--	--	280	--
														280
0010	2	206+75	-	214+15	LT	--	--	--	--	--	--	25	--	245
SUBTOTALS						--	--	--	--	--	--	25	--	245
														245
0010	3	199+36	-	214+75	LT & RT	2,480	160	25	5	45	1,870	220	1,060	420
SUBTOTALS						2,480	160	25	5	45	1,870	220	1,060	420
														2,135
0010	6	145+50	-	146+50	LT	--	--	--	--	--	--	--	--	200
SUBTOTALS						--	--	--	--	--	--	--	--	200
														200
TOTALS						2,535	160	25	5	2,160			2,205	

LONG LINE PAVEMENT MARKING ITEMS														
CATEGORY	STAGE	STATION	TO	STATION	OFFSET	646.1020	646.3020		646.4520		646.9010	646.9110	646.9210	
						MARKING LINE EPOXY 4-INCH	MARKING LINE EPOXY 8-INCH		MARKING LINE SAME DAY EPOXY 4-INCH		MARKING REMOVAL LINE WATER BLASTING 4-INCH LF	MARKING REMOVAL LINE WATER BLASTING 8-INCH LF	MARKING REMOVAL LINE WATER BLASTING WIDE LF	
							12.5' LINE 37.5' SKIP WHITE LF	SOLID WHITE LF	WHITE LF	YELLOW LF			12.5' LINE 37.5' SKIP YELLOW LF	SOLID YELLOW LF
0010	3	199+45	-	201+25	LT & RT	--	--	--	--	--	130	75	25	--
	5	199+45	-	210+59	LT & RT	198	950	315	30	150	2,378	165	80	310
	5	210+59	-	220+00	LT & RT	102	--	--	--	--	1,882	--	--	--
	7	220+00	-	223+75	LT & RT	--	250	15	--	--	1,025	--	--	--
TOTALS						300	1,200	330	30	150	5,300	1,320	155	335
														1,500
														360
														5,450

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MISCELLANEOUS PAVEMENT MARKING ITEMS													
						646.5020	646.5120	646.7120	646.7420	646.7520	646.8120	646.8220	646.9310
CATEGORY	STAGE	STATION	TO	STATION	OFFSET	MARKING ARROW EPOXY	MARKING WORD EPOXY	MARKING DIAGONAL EPOXY 12-INCH	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH	MARKING CROSSWALK EPOXY LADDER PATTERN 24-INCH	MARKING CURB EPOXY	MARKING ISLAND NOSE EPOXY	MARKING REMOVAL SPECIAL MARKING WATER BLASTING
						EACH	EACH	WH I T E L F	Y E L L O W L F	L F	L F	L F	EACH
0010	3	199+45	-	201+25	LT & RT	-	--	-	-	-	-	-	5
	5 & 7	199+45	-	223+72	LT & RT	9	2	100	200	180	200	55	2
TOTALS						9	2	100	200	180	200	55	2
								300					

3

CONSTRUCTION STAKING ITEMS

						650.4000	650.5000	650.5500	650.8000	650.8500	650.9910	650.9920
						CONSTRUCTION STAKING STORM SEWER	CONSTRUCTION STAKING BASE	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER	CONSTRUCTION STAKING RESURFACING REFERENCE	CONSTRUCTION STAKING ELECTRICAL INSTALLATIONS (6999-03-70)	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (6999-03-70)	CONSTRUCTION STAKING SLOPE STAKES
CATEGORY	STAGE	STATION	TO	STATION	OFFSET	EACH	LF	LF	LF	LS	LS	LF
0010												
	1	201+25	-	224+40	LT & RT	--	--	--	2,315	--	--	--
	3	201+25	-	210+59	LT & RT	3	934	934	--	--	--	934
	3	203+50	-	204+00	RT	--	--	36	--	--	--	--
	3	206+72	-	207+21	RT	--	--	90	--	--	--	--
PROJECT					--	--	--	--	1	1	--	
TOTALS						3	934	1,060	2,315	1	1	934

LIGHTING CONDUIT AND WIRE ITEMS

				652.0325	655.0630
				CONDUIT RIGID NONMETALLIC SCHEDULE 80 2-INCH L F	ELECTRICAL WIRE LIGHTING 4 AWG L F
CATEGORY	STAGE	FROM	TO		
0010	3	EX SL	- SL 100	140	1,050
	3	SL 100	- SL 101	190	1,400
	3	SL 101	- SL 102	215	1,575
	3	SL 102	- SL 103	200	1,470
	3	SL 103	- SL 104	200	1,470
	3	SL 104	- EX SL	190	1,400
TOTALS				1,135	8,365

*CONDUCTOR QUANTITIES BASED ON AS-BUILT PLANS, WHICH SUGGEST 7 - #4 CONDUCTORS PER LINEAR FOOT OF CONDUIT

STREET LIGHTING ASSEMBLIES

					655.0610	SPV.0060.04
					ELECTRICAL WIRE LIGHTING 12 AWG L F	REMOVE AND REINSTALL STREET LIGHT ASSEMBLY EACH
CATEGORY	STAGE	LIGHT NUMBER	STATION	OFFSET		
0010	3	SL100	202+24	31.5' RT	270	1
	3	SL101	204+13	31.5' RT	270	1
	3	SL102	206+25	31.5' RT	270	1
	3	SL103	208+25	27.7' RT	270	1
	3	SL104	210+25	26.3' RT	270	1
TOTALS					1,350	5

SAWING ITEMS

						690.0150	690.0250	COMMENTS
						SAWING ASPHALT L F	SAWING CONCRETE L F	
CATEGORY	STAGE	STATION	TO	STATION	OFFSET			
0010	1	206+72	-	207+21	LT	125	-	
	3	201+25	-	210+59	LT & RT	1,140	15	
	3	203+50	-	204+50	RT	72	5	
	3	206+91	-	207+02	RT	11	-	
	4 & 5	201+25	-	210+59	LT & RT	254	-	MANHOLE ADJUSTMENTS
	5	206+30	-	206+56	LT	26	-	WOODBINE RD
TOTALS	6	222+55	-	224+40	LT	22	105	
						1,650	125	

MONUMENT ITEMS

				SPV.0060.05	SPV.0060.06
				RESEARCH AND LOCATE EXISTING LAND PARCEL MONUMENTS EACH	VERIFY AND REPLACE EXISTING LAND PARCEL MONUMENTS EACH
CATEGORY	STAGE	STATION	LOCATION		
0010	3	204+05	RT	1	1
	3	205+25	RT	1	1
TOTALS				2	2

PROJECT NO:6999-03-70

HWY: STH 54

COUNTY: WOOD

MISCELLANEOUS QUANTITIES

SHEET

E

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
TRANSPORTATION PROJECT PLAT TITLE SHEET
PROJECT NO. 6999-03-20
City of Wisconsin Rapids, Second Ave South
GAYNOR AVENUE TO STH 13

STH 54
WOOD COUNTY



CONVENTIONAL SYMBOLS

SECTION LINE	SECTION CORNER	R/W MONUMENT
QUARTER LINE	NOTATION FOR COMBUSTIBLE FLUIDS	NON-MONUMENTED R/W POINT
SIXTEENTH LINE	NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES	FOUND IRON PIN
NEW REFERENCE LINE		VALVE (GAS, WATER, ETC.)
NEW R/W LINE		SIGN
EXISTING R/W LINE		OFF-PREMISE SIGN
PROPERTY LINE		
LOT, TIE & OTHER MINOR LINES		
CORPORATE LIMITS		
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	ELECTRIC POLE TELEPHONE POLE	COMPENSABLE
FEE ACQUISITION AREA (HATCHING VARIES BY OWNER)	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)	NON-COMPENSABLE
TEMPORARY LIMITED EASEMENT AREA		
EASEMENT AREA (HIGHWAY, PERMANENT LIMITED, OR RESTRICTED DEVELOPMENT)	ACCESS CONTROLLED BY ACQUISITION	
TRANSMISSION STRUCTURES	NO ACCESS (BY STATUTORY AUTHORITY)	
BUILDING	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	
NATIONAL GEODETIC SURVEY MONUMENT		PARCEL NUMBER
SIXTEENTH CORNER MONUMENT		UTILITY NUMBER

CONVENTIONAL ABBREVIATIONS

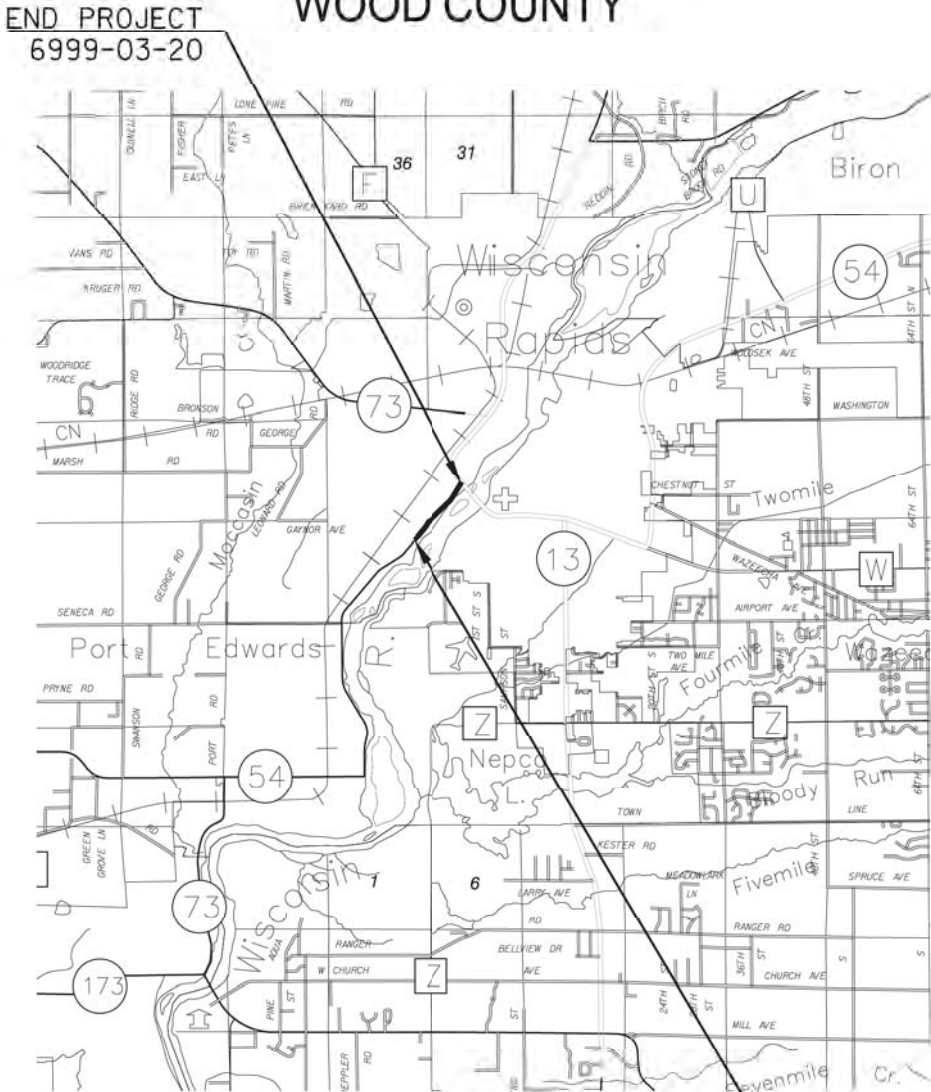
ACCESS RIGHTS	AR	POINT OF COMPOUND CURVE	PCC
ACRES	AC	POINT OF INTERSECTION	PI
AHEAD	ΔH	PROPERTY LINE	PL
ALUMINUM	ALUM	RECORDED AS	(100')
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RIGHT	RT
CENTERLINE	C/L	RIGHT OF WAY	R/W
CERTIFIED SURVEY MAP	CSM	SECTION	SEC
CONCRETE	CONC	SEPTIC VENT	SEPV
COUNTY	CO	SQUARE FEET	SF
COUNTY TRUNK HIGHWAY	CTH	STATE TRUNK HIGHWAY	STH
DISTANCE	DIST	STATION	STA
CORNER	COR	SUBDIVISION	SUBD
DOCUMENT NUMBER	DOC	TANGENT	TAN
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED EASEMENT	TLE
GAS VALVE	GV		
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP
HIGHWAY EASEMENT	HE	PLAT	
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		

CURVE DATA

LONG CHORD	LC
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE OR DELTA	Δ
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

WATER	—W—
GAS	—G—
TELEPHONE	—T—
OVERHEAD	—OH—
TRANSMISSION LINES	—E—
ELECTRIC	—TV—
CABLE TELEVISION	—FD—
FIBER OPTIC	—SAN—
SANITARY SEWER	—SS—
STORM SEWER	



THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 6999-03-20

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, WOOD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBAR), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLES) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPANCIAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS & INTERESTS REQUIRED.

EXISTING ACCESS CONTROL ALONG STH 54 IS SHOWN FROM PREVIOUS RW PROJECT 6140-01-21 & 6999-05-61.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:

STH 54
RW PROJECTS 6140-01-21, 6390-00-22, 6999-05-61, WEST SIDE ASSESSOR'S PLAT NO 17, RIVER PARK ADDITION TO CITY OF GRAND RAPIDS, LYON PARK ADDITION TO GRAND RAPIDS, CSM 9340, CSM 7205, CSM 7787, CSM 1287 & CSM 98
WOODBINE RD
WEST SIDE ASSESSOR'S PLAT NO 17
LYON ST
RW PROJECT 6140-01-21, 6999-05-61 & RIVER PARK ADDITION TO CITY OF GRAND RAPIDS

BEGIN PROJECT
6999-03-20

LAYOUT
SCALE 0 1 MI.

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 6999-03-20 - 4.01
SHEET 2 OF 2
AMENDMENT NO:

TRANSPORTATION PROJECT PLAT NO: 6999-03-20 - 4.02

THAT PART OF LOTS 3,4 AND 24 OF WEST SIDE ASSESSOR'S PLAT NO. 17, RECORDED IN V. 12, P. 155, DOC. 743182, LOCATED IN AND PART OF GOVERNMENT LOT 4, SECTION 18, TOWNSHIP 22 NORTH, RANGE 6 EAST, CITY OF WISCONSIN RAPIDS, WOOD COUNTY, WISCONSIN.

RELOCATION ORDER STH 54 WOOD COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE CITY OF WISCONSIN RAPIDS DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

EFFECT THIS TO CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 62.22, WISCONSIN STATUTES, THE CITY OF WISCONSIN RAPIDS

HEREBY ORDERS THAT:
1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE CITY FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE CITY OF WISCONSIN RAPIDS, PURSUANT TO THE PROVISIONS OF SECTION 62.22, WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

PARCEL NUMBER	OWNERS	INTEREST REQUIRED	R/W	ACRES REQUIRED	TLE
3	CITY OF WISCONSIN RAPIDS	FEE/TLE	0.046	---	0.069
4	DANIEL L. MORZEWSKI & SUSAN M. MORZEWSKI	FEE/TLE	0.003	---	0.012

POINT	STATION	OFFSET
200	205+25.11	0.00'
201	205+25.11	36.10' LT
202	206+68.39	36.14' LT
203	206+68.39	36.15' LT
204	207+68.39	36.19' LT
205	209+68.37	35.74' LT
206	210+00.00	35.77' LT
207	210+00.00	0.00'
208	210+00.00	30.03' RT
209	209+69.26	29.82' RT
210	205+25.11	29.90' RT
PRW220	209+43.50	35.80' LT
PRW221	209+43.50	46.00' LT
PRW222	209+56.50	46.00' LT
PRW223	209+56.50	35.76' LT
PRW224	207+58.00	29.81' RT
PRW225	207+05.00	39.50' RT
PRW226	209+25.12	39.50' RT

POINT	STATION	OFFSET
TLE230	209+33.50	35.82' LT
TLE231	209+33.50	56.00' LT
TLE232	209+66.50	56.00' LT
TLE233	209+66.50	35.74' LT
TLE234	209+00.00	29.81' RT
TLE235	208+45.00	36.00' RT
TLE236	208+10.00	36.00' RT
TLE237	207+75.00	32.00' RT
TLE238	207+23.00	36.21' RT
TLE239	207+05.00	51.00' RT
TLE240	205+90.00	54.00' RT
TLE241	205+25.13	48.00' RT

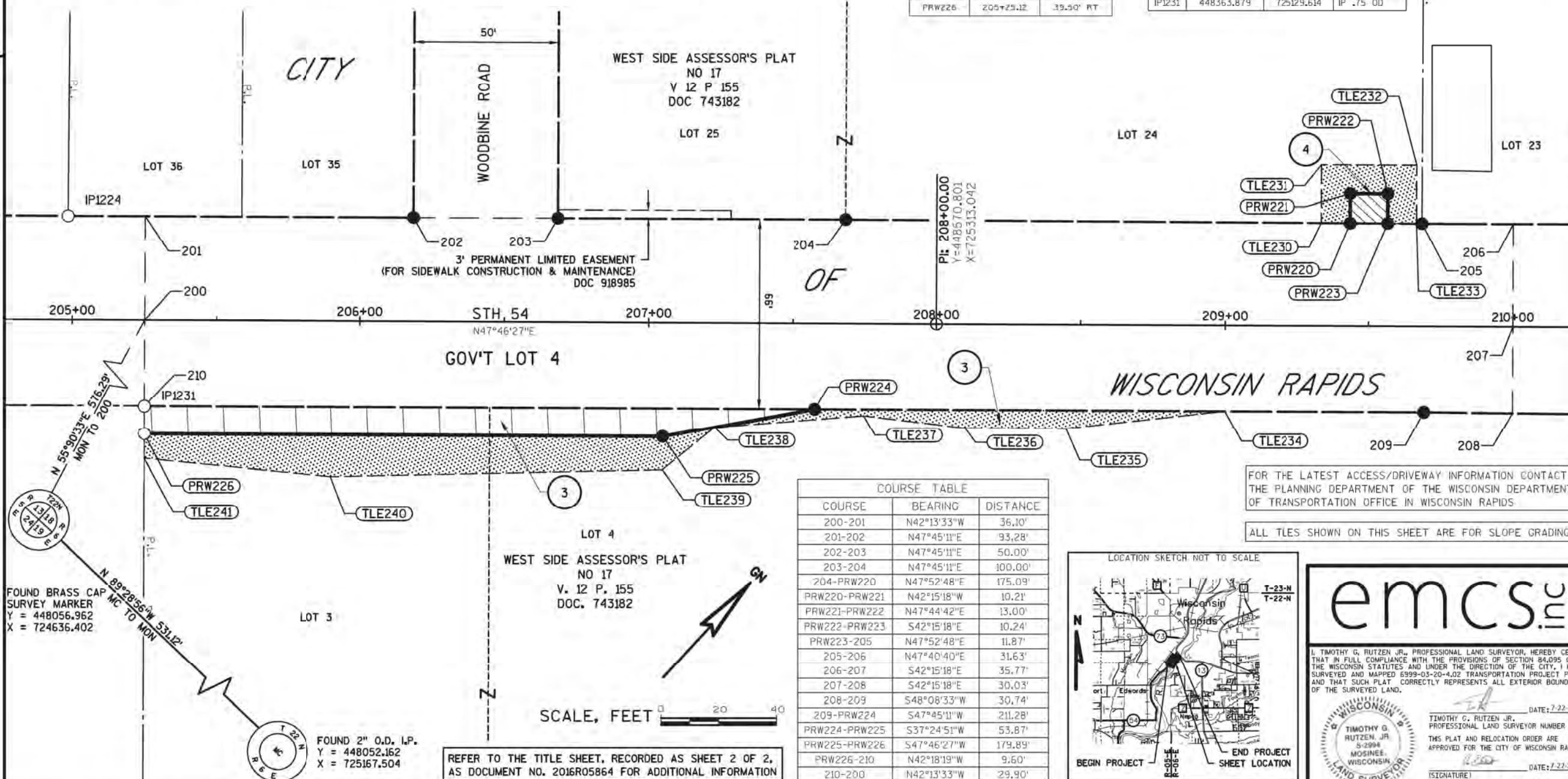
POINT	NORTHING (Y)	EASTING (X)	DESCRIPTION
IP1224	448394.839	725065.435	IP 125 OD
IP1231	448363.879	725129.614	IP .75 OD



2016R05888

SUSAN E. GINTER
WOOD COUNTY
REGISTER OF DEEDS
RECORDED ON
07/22/2016 01:15PM
REC FEE: 25.00
EXEMPT #: 1
PAGES: 1

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 6999-03-20-4.02
AMENDMENT NO. _____



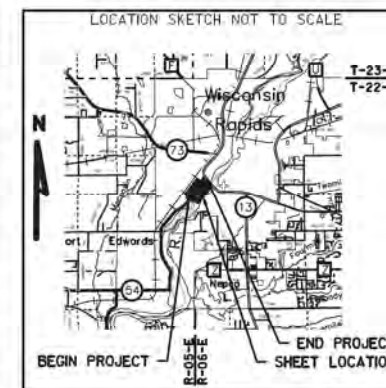
FOUND BRASS CAP
SURVEY MARKER
Y = 448056.962
X = 724636.402

FOUND 2" O.D. I.P.
Y = 448052.162
X = 725167.504

REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2,
AS DOCUMENT NO. 2016R05864 FOR ADDITIONAL INFORMATION

COURSE TABLE

COURSE	BEARING	DISTANCE
200-201	N42°13'33"W	36.10'
201-202	N47°45'11"E	93.28'
202-203	N47°45'11"E	50.00'
203-204	N47°45'11"E	100.00'
204-PRW220	N47°52'48"E	175.09'
PRW220-PRW221	N42°15'18"W	10.21'
PRW221-PRW222	N47°44'42"E	13.00'
PRW222-PRW223	S42°15'18"E	10.24'
PRW223-205	N47°52'48"E	11.87'
205-206	N47°40'40"E	31.63'
206-207	S42°15'18"E	35.77'
207-208	S42°15'18"E	30.03'
208-209	S48°08'33"W	30.74'
209-PRW224	S47°45'11"W	211.28'
PRW224-PRW225	S37°24'51"W	53.87'
PRW225-PRW226	S47°46'27"W	179.89'
PRW226-210	N42°18'13"W	9.60'
210-200	N42°13'33"W	29.90'



emcs

I, TIMOTHY G. RUTZEN JR., PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE CITY, I HAVE SURVEYED AND MAPPED 6999-03-20-4.02 TRANSPORTATION PROJECT PLAT AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

TIMOTHY G. RUTZEN JR.
PROFESSIONAL LAND SURVEYOR NUMBER 2994
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE CITY OF WISCONSIN RAPIDS.

DATE: 7-22-2016
DATE: 7-22-2016
PRINTED NAME: JOE EICHSTEADT

TRANSPORTATION PROJECT PLAT NO: 6999-03-20 - 4.03
OF BLOCK 3

THAT PART OF LOTS 2, 3, 4, AND 5 OF RIVER PARK ADDITION TO THE CITY OF GRAND RAPIDS, RECORDED IN V. 3, P. 33, DOC. 89594, LOCATED IN AND PART OF GOVERNMENT LOT 3, SECTION 18, TOWNSHIP 22 NORTH, RANGE 6 EAST, CITY OF WISCONSIN RAPIDS, WOOD COUNTY, WISCONSIN.

RELOCATION ORDER STH 54 WOOD COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE CITY OF WISCONSIN RAPIDS DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

EFFECT THIS TO CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 62.22, WISCONSIN STATUTES, THE CITY OF WISCONSIN RAPIDS HEREBY ORDERS THAT:
1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE CITY FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE CITY OF WISCONSIN RAPIDS, PURSUANT TO THE PROVISIONS OF SECTION 62.22, WISCONSIN STATUTES.

COURSE TABLE		
COURSE	BEARING	DISTANCE
300-301	N61°52'39"W	37.13'
301-302	N29°17'12"E	209.08'
302-303	N22°32'15"E	50.32'
303-304	S60°42'48"E	44.44'
304-305	SEE CURVE DATA ON SHEET	
305-300	S28°07'21"W	72.41'

2016R05887
SUSAN E. GINTER
WOOD COUNTY
REGISTER OF DEEDS
RECORDED ON
07/22/2016 01:15PM
REC FEE: 25.00
EXEMPT #: P
PAGES: 1 emcs Inc

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 6999-03-20-4.03
AMENDMENT NO. _____

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION CONTACT
THE PLANNING DEPARTMENT OF THE WISCONSIN DEPARTMENT
OF TRANSPORTATION OFFICE IN WISCONSIN RAPIDS

REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2,
AS DOCUMENT NO. 2016R05864 FOR ADDITIONAL INFORMATION

CURVE 2 DATA

PI STA = 225+17.32
Y = 449917.83
X = 726349.80
PC STA = 223+22.41
Y = 449745.93
X = 726257.92
PT STA = 227+11.51
Y = 450074.1141
X = 726466.28
DELTA = 8°34'28"
D = 2°12'13"
T = 194.91'
L = 389.10'
R = 2600.00'

EXISTING MONUMENTS			
POINT	NORTHING (Y)	EASTING (X)	DESCRIPTION
IP1200	449662.754	726170.402	IP 4X4 CONCRETE W/ROD
IP1203	449860.155	726281.118	IP LO IR00

RIVER PARK ADDITION
TO CITY OF GRAND RAPIDS
V 3 P 33
DOC 89594
BLOCK 1

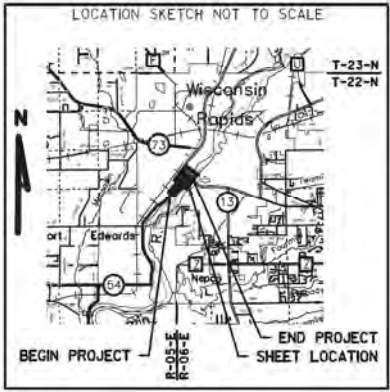
WISCONSIN RAPIDS

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

PARCEL NUMBER	OWNERS	INTEREST REQUIRED	R/W NEW	ACRES REQUIRED EXISTING	TOTAL
6	MICHAEL J & MARLENE A WINTERS OR ANY SUCCESSOR TRUSTEE, TRUSTEES OF THE MICHAEL J. WINTERS AND MARLENE A. WINTERS REVOCABLE TRUST DATED SEPT. 24, 1997	AR	0.000	---	0.000
7	JOSEPH J AND PATRICIA A KOZICKI TRUSTEES OF THE JOSEPH J. KOZICKI AND PATRICIA A. KOZICKI REVOCABLE TRUST AGREEMENT DATED SEPT. 11, 2014	AR	0.000	---	0.000
8	RICHARD L PREWITT	AR	0.000	---	0.000
9	DNA INVESTMENTS LLC	AR	0.000	---	0.000

RW STATION & OFFSET TABLE		
POINT	STATION	OFFSET
300	222+50.00	0.00'
301	222+50.00	37.13' LT
302	224+57.21	36.42' LT
303	225+06.14	44.38' LT
304	225+08.37	0.00'
305	223+22.41	0.00'

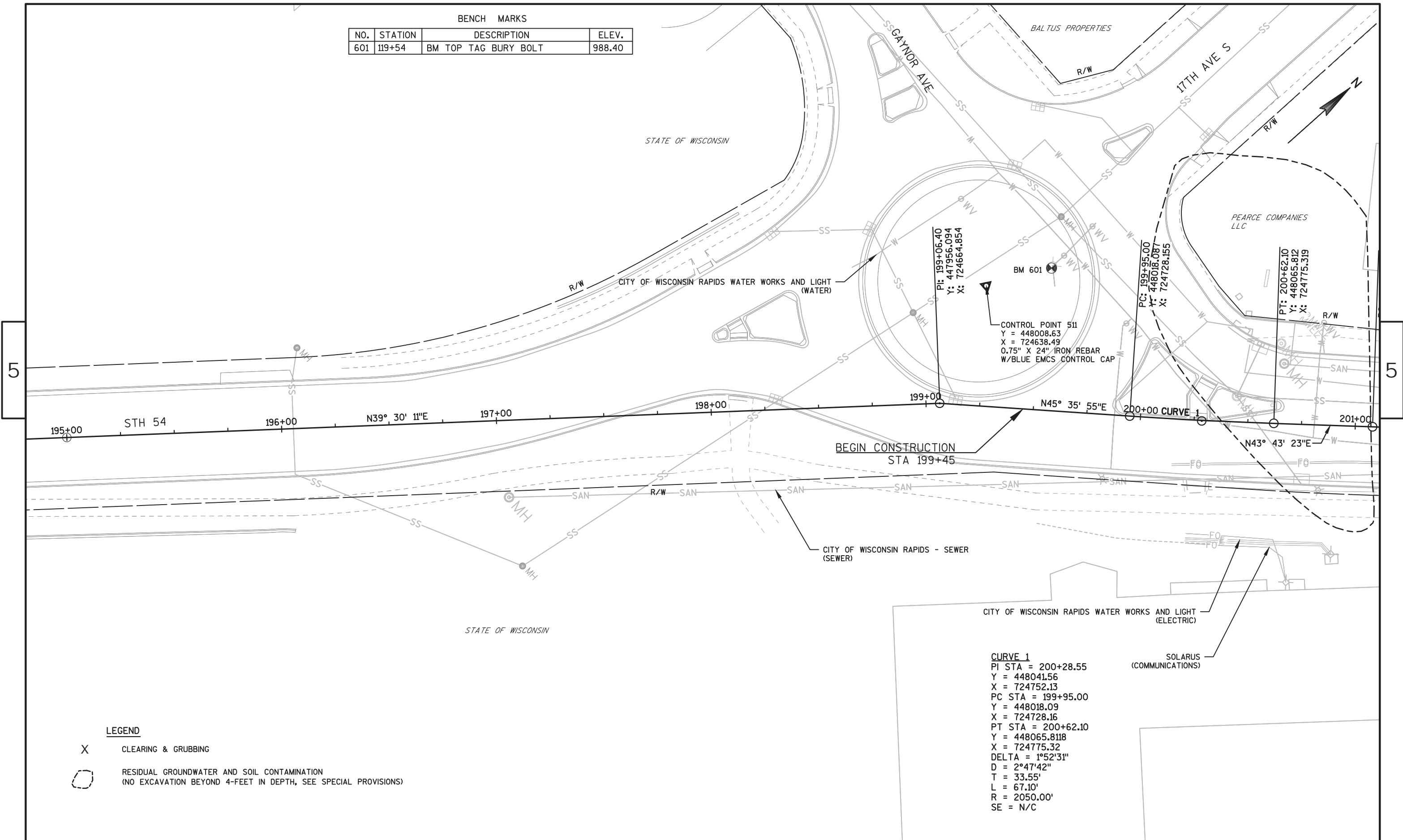


SCALE, FEET 0 20 40

emcs Inc

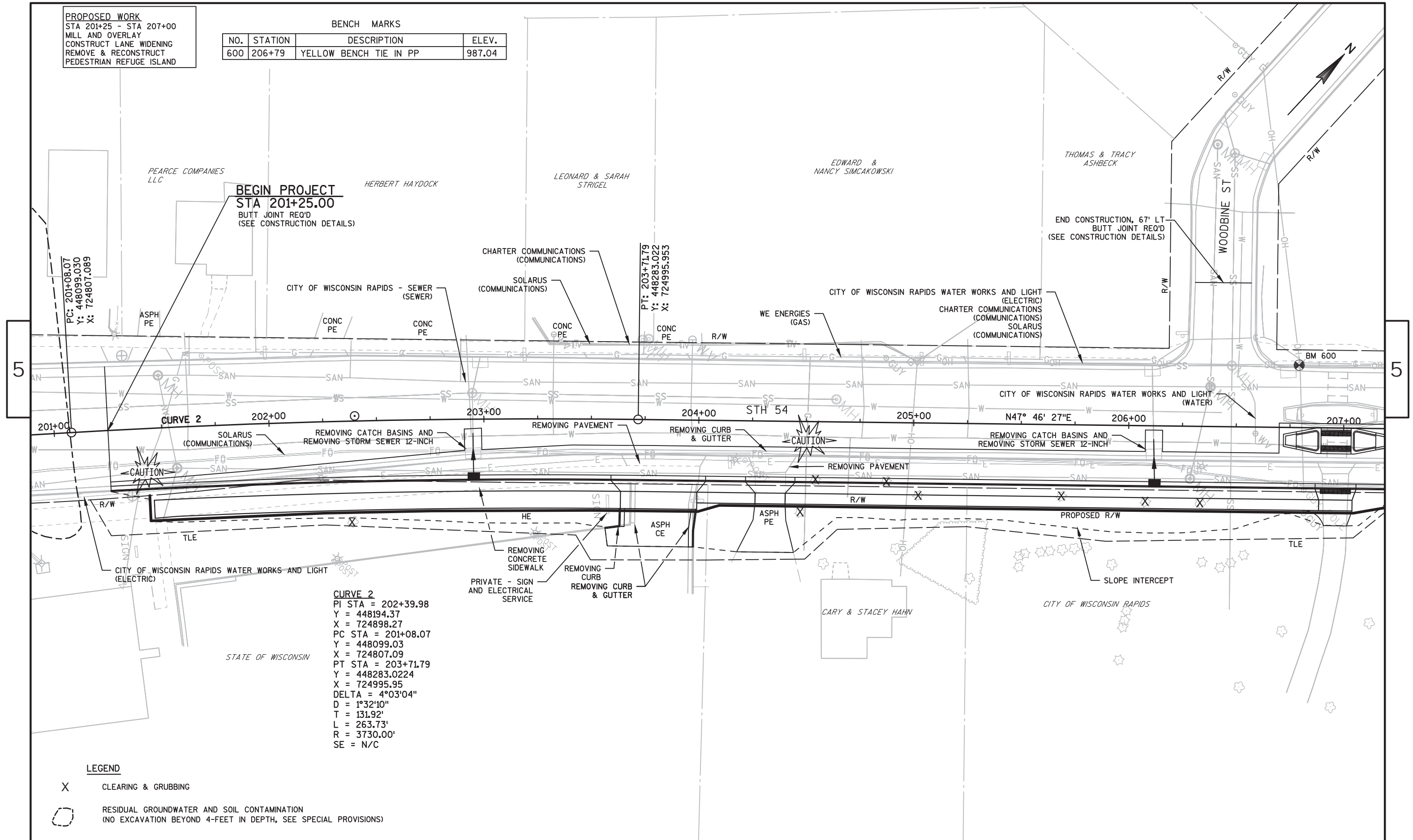
I, TIMOTHY G. RUTZEN JR., PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.05 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE CITY, I HAVE SURVEYED AND MAPPED 6999-03-20-4.03 TRANSPORTATION PROJECT PLAT AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.
TIMOTHY G. RUTZEN JR.
S-2994
MOSINEE, WISCONSIN
LAND SURVEYOR
DATE: 7-22-2016
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE CITY OF WISCONSIN RAPIDS.
DATE: 7-22-2016
SIGNATURE: [Signature]
PRINTED NAME: JOE EICHSTEADT

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
601	119+54	BM TOP TAG BURY BOLT	988.40



PROPOSED WORK
STA 201+25 - STA 207+00
MILL AND OVERLAY
CONSTRUCT LANE WIDENING
REMOVE & RECONSTRUCT
PEDESTRIAN REFUGE ISLAND

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
600	206+79	YELLOW BENCH TIE IN PP	987.04



CURVE 2
PI STA = 202+39.98
Y = 448194.37
X = 724898.27
PC STA = 201+08.07
Y = 448099.03
X = 724807.09
PT STA = 203+71.79
Y = 448283.0224
X = 724995.95
DELTA = 4°03'04"
D = 1°32'10"
T = 131.92'
L = 263.73'
R = 3730.00'
SE = N/C

LEGEND

- X CLEARING & GRUBBING
- RESIDUAL GROUNDWATER AND SOIL CONTAMINATION
(NO EXCAVATION BEYOND 4-FEET IN DEPTH, SEE SPECIAL PROVISIONS)

PROJECT NO: 6999-03-70

HWY: STH 54

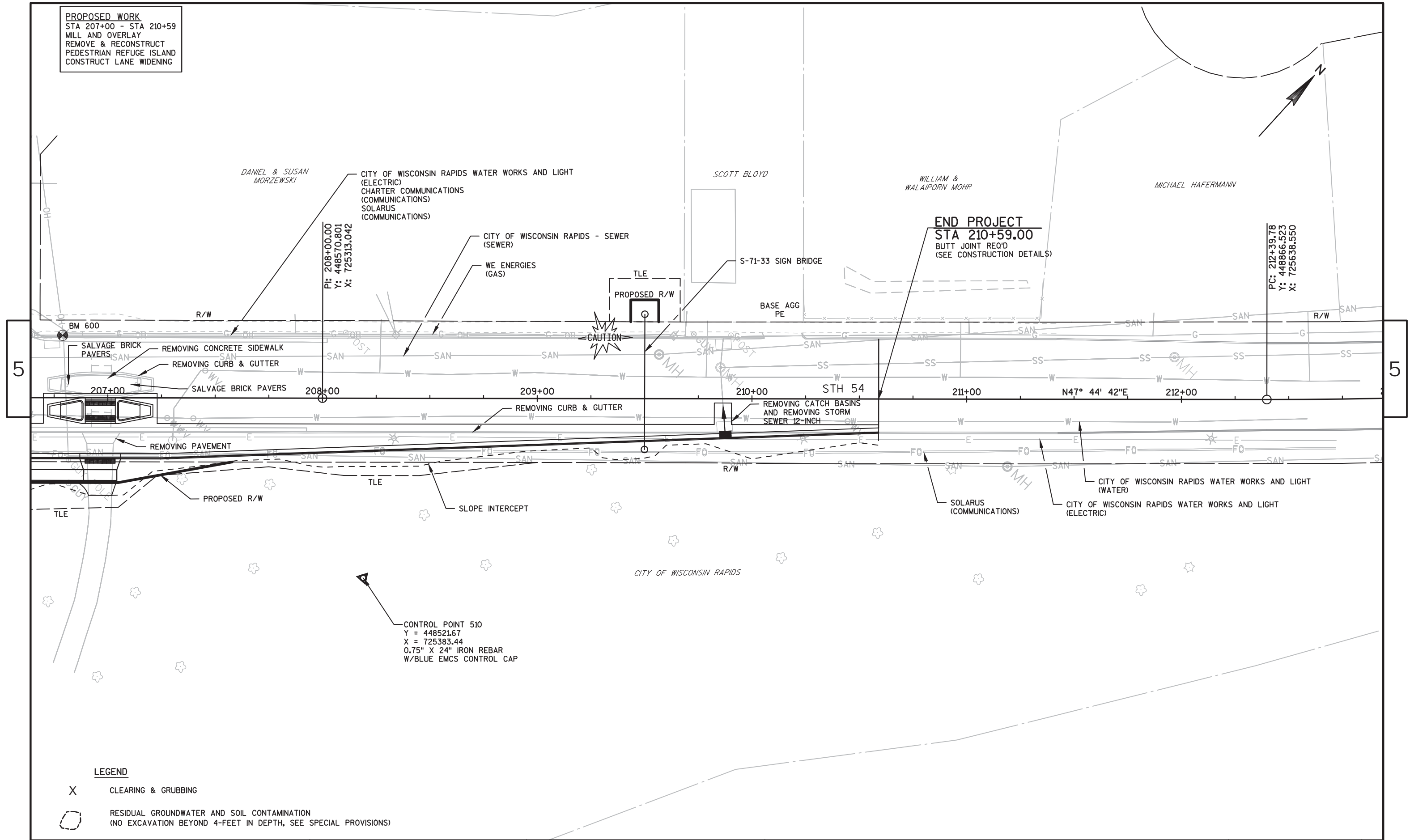
COUNTY: WOOD

PLAN

SHEET

E

PROPOSED WORK
STA 207+00 - STA 210+59
MILL AND OVERLAY
REMOVE & RECONSTRUCT
PEDESTRIAN REFUGE ISLAND
CONSTRUCT LANE WIDENING



LEGEND

- X CLEARING & GRUBBING
- RESIDUAL GROUNDWATER AND SOIL CONTAMINATION
(NO EXCAVATION BEYOND 4-FEET IN DEPTH, SEE SPECIAL PROVISIONS)

PROJECT NO: 6999-03-70

HWY: STH 54

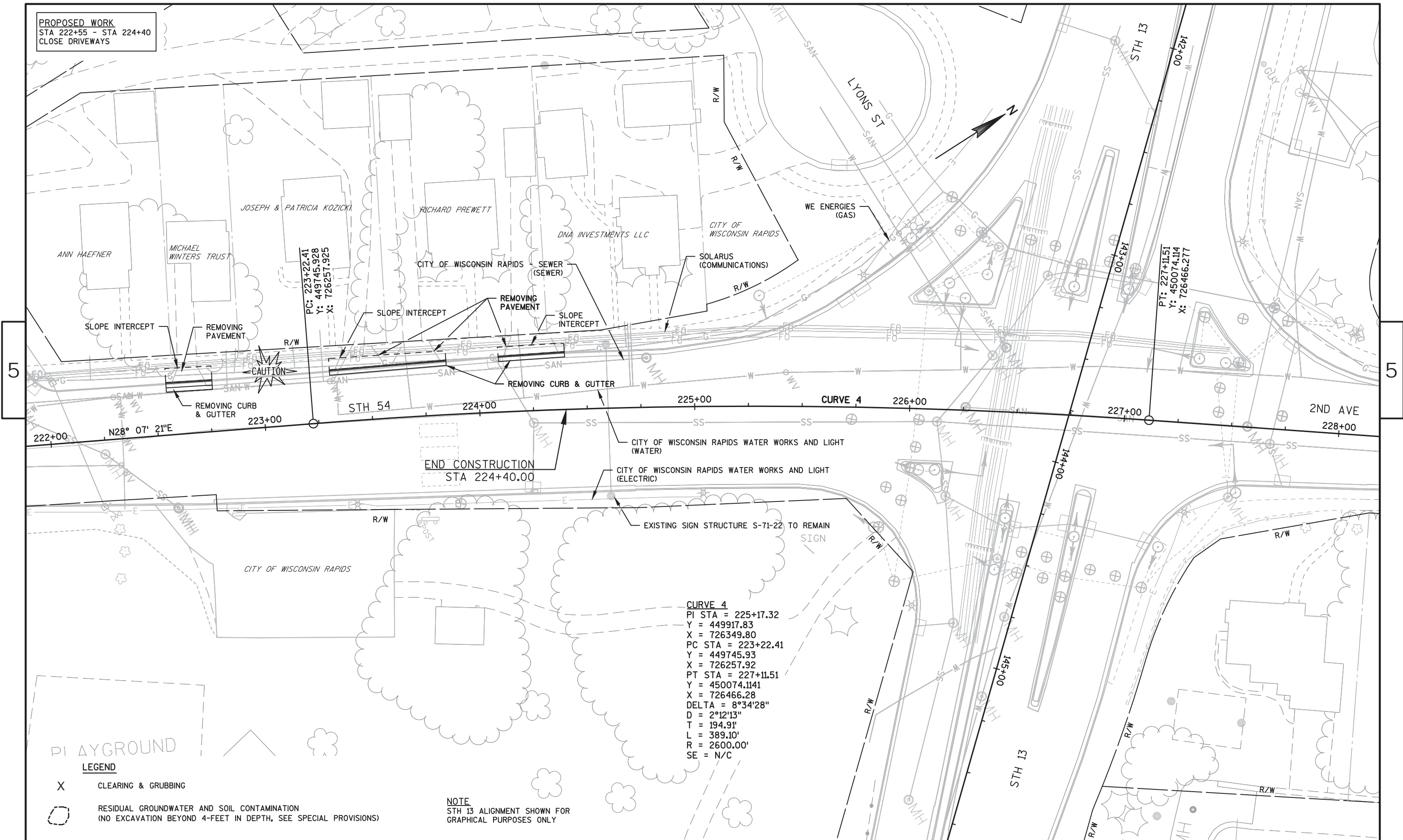
COUNTY: WOOD

PLAN

SHEET

E

PROPOSED WORK
STA 222+55 - STA 224+40
CLOSE DRIVEWAYS



END CONSTRUCTION
STA 224+40.00

CURVE 4
PI STA = 225+17.32
Y = 449917.83
X = 726349.80
PC STA = 223+22.41
Y = 449745.93
X = 726257.92
PT STA = 227+11.51
Y = 450074.1141
X = 726466.277
DELTA = 8°34'28"
D = 2°12'13"
T = 194.91'
L = 389.10'
R = 2600.00'
SE = N/C

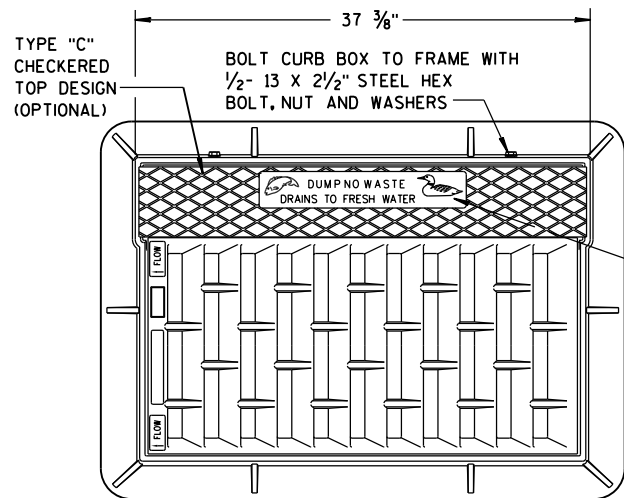
PI DAYGROUND
LEGEND

- X CLEARING & GRUBBING
- RESIDUAL GROUNDWATER AND SOIL CONTAMINATION
(NO EXCAVATION BEYOND 4-FEET IN DEPTH, SEE SPECIAL PROVISIONS)

NOTE
STH 13 ALIGNMENT SHOWN FOR
GRAPHICAL PURPOSES ONLY

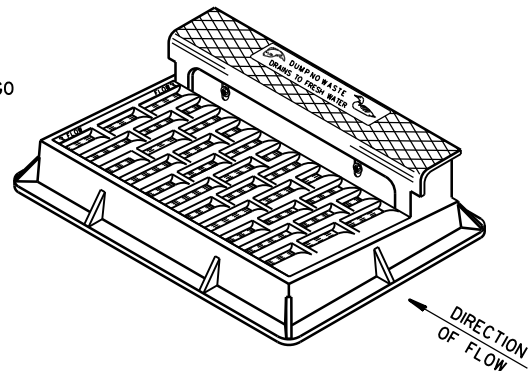
Standard Detail Drawing List

08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A09-02	CATCH BASINS 2X3-FT AND 2.5X3-FT
08D01-20A	CONCRETE CURB & GUTTER
08D01-20B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-19A	CURB RAMPS TYPES 1 AND 1-A
08D05-19B	CURB RAMPS TYPES 2 AND 3
08D05-19C	CURB RAMPS TYPES 4A AND 4A1
08D05-19D	CURB RAMPS TYPE 4B AND 4B1
08D05-19E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D18-02	DRIVEWAY AND SIDEWALK RAMPS TYPES X & Y
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09B02-10	CONDUIT
09B04-11	PULL BOX
09E03-05	NON-FREEWAY LIGHTING UNIT POLE WIRING
11B02-02	CONCRETE MEDIAN NOSE
12A04-03	STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES & OVERHEAD SIGN SUPPORTS & TRAFFIC SIGNALS
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C07-14B	PAVEMENT MARKING WORDS
15C07-14C	PAVEMENT MARKING ARROWS
15C08-19A	LONGITUDINAL MARKING (MAINLINE)
15C08-19B	PAVEMENT MARKING (TURN LANES)
15C08-19C	PAVEMENT MARKING (TURN LANES)
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-06	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C15-05	36" DIAMETER FULL SPAN OVERHEAD SIGN SUPPORT BASE
15C18-04	MEDIAN ISLAND MARKING
15C19-05B	MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY
15C27-03B	PAVEMENT MARKING (ISLANDS)
15C33-03	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D20-04	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D21-06	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D28-03	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-04A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-04B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-04C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS

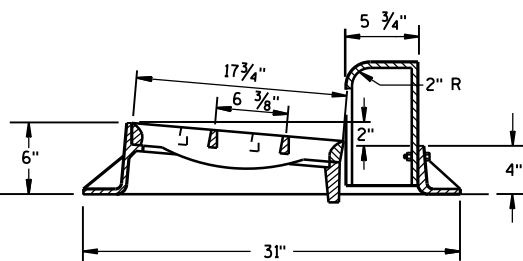
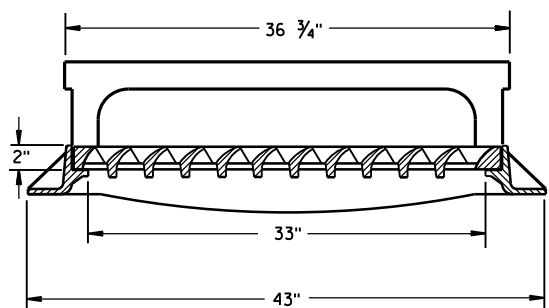
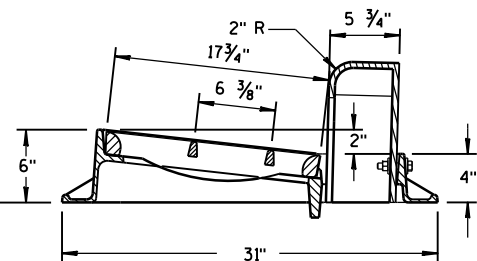
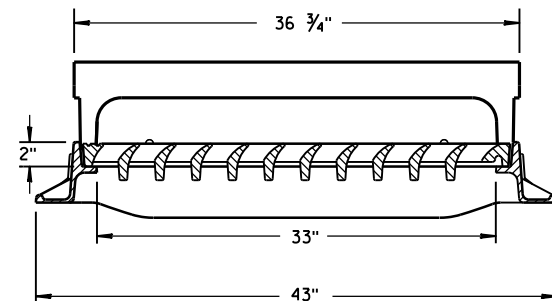
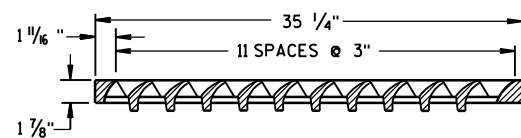


SEE LOGO
DETAIL

NOTE:
GRATE IS REVERSIBLE.

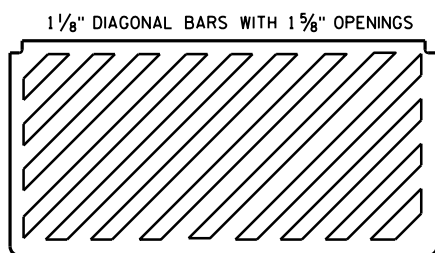


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"



TYPE "H"

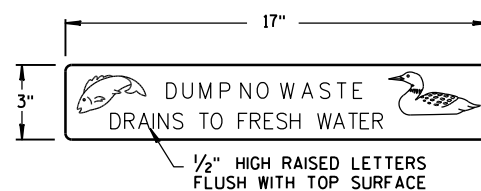
NOTE: EITHER CASTING IS ACCEPTABLE



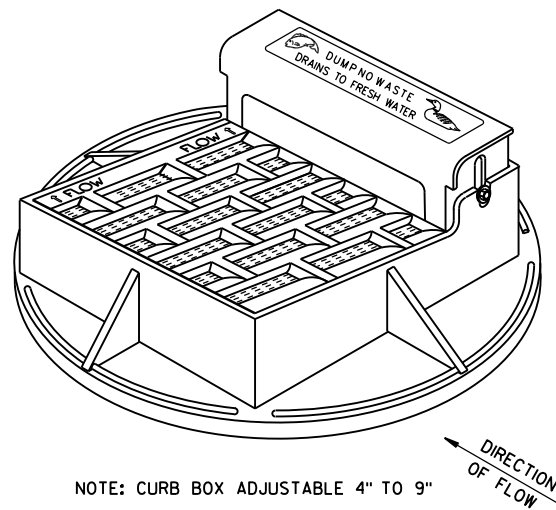
**SPECIAL GRATE FOR
TYPE "H" COVER**

(MEASURES 35 1/4" X 17 3/4" X 2")

(NOTED AS TYPE H-S ON DRAINAGE TABLE)

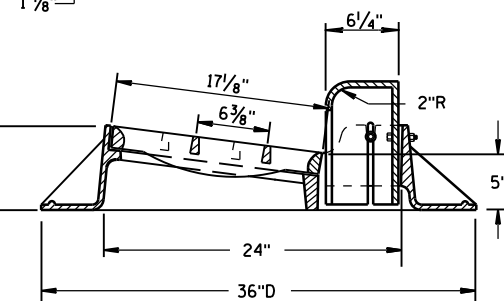
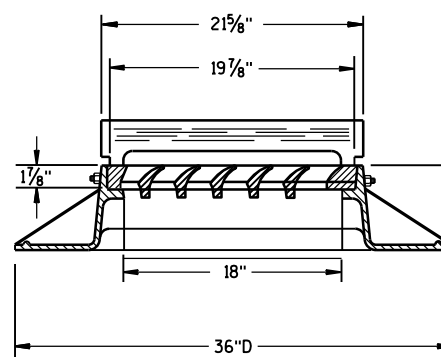
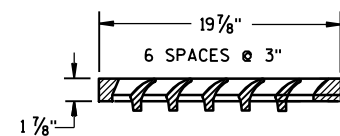
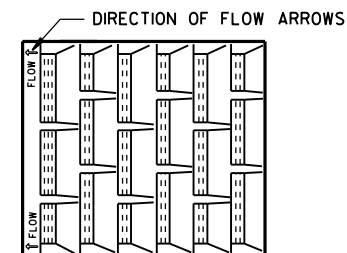


LOGO DETAIL

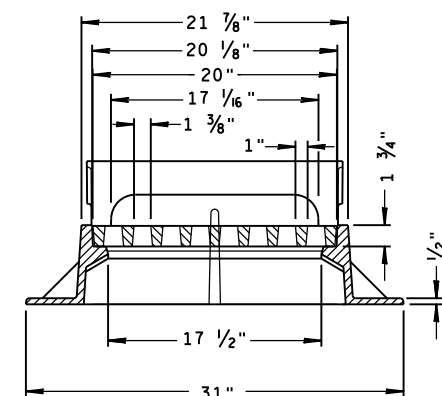
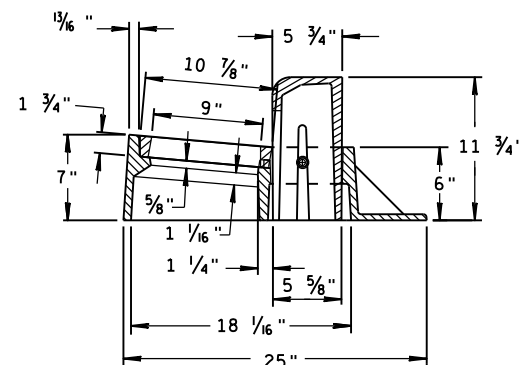


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

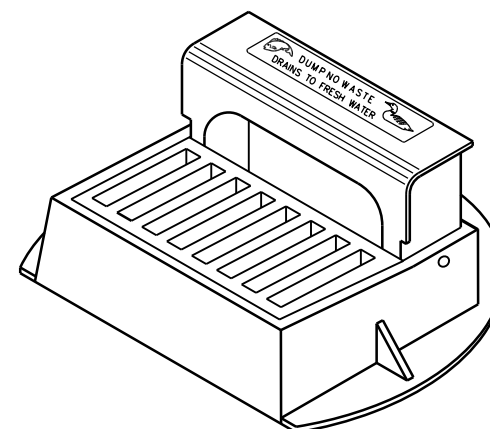
NOTE:
GRATE IS REVERSIBLE.



TYPE "A"



TYPE "Z"

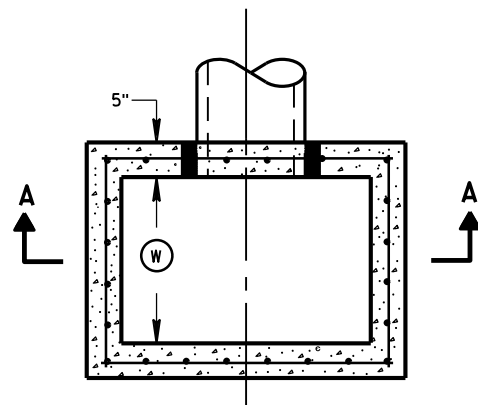


**INLET COVERS
TYPE A, H, A-S, H-S & Z**

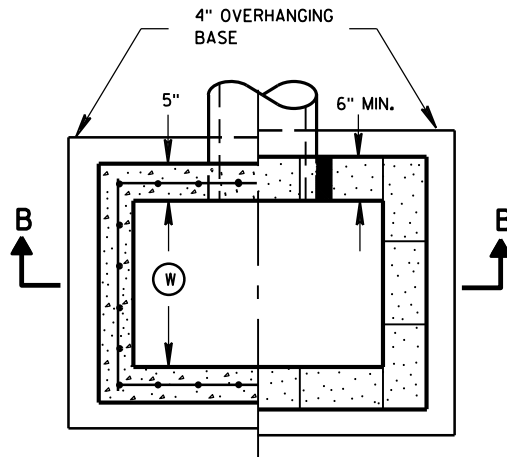
**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

APPROVED
11-27-13
DATE
FHWA

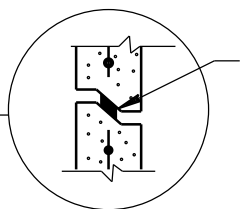
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



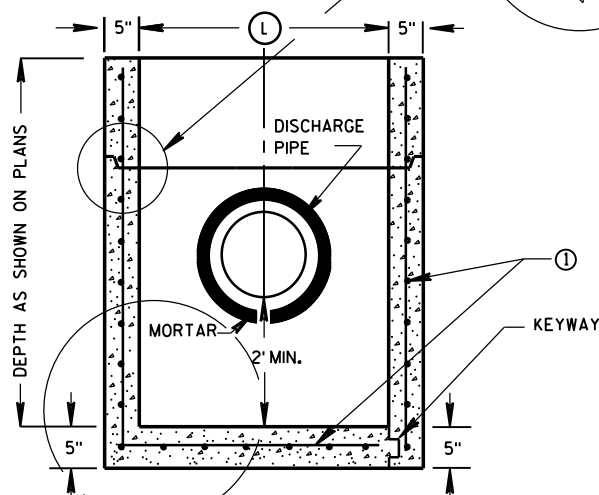
PLAN VIEW



PLAN VIEW



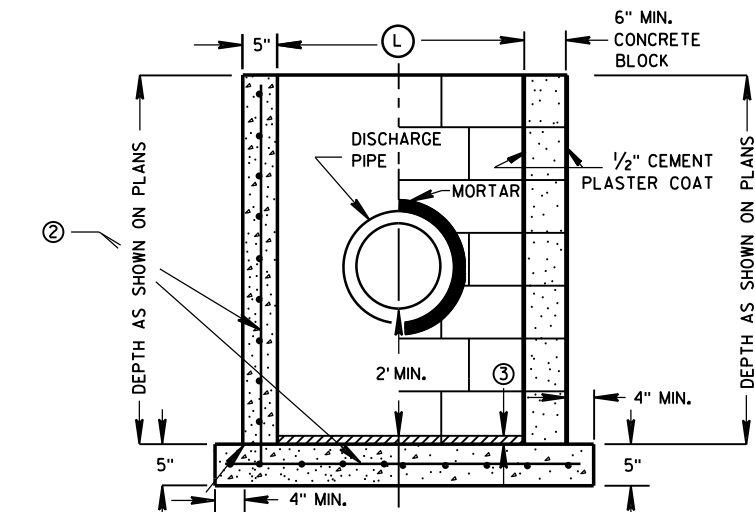
RISER JOINTS TO BE SEALED
 WITH A BUTYL RUBBER SEAL
 PER SEALANT MANUFACTURERS
 RECOMMENDATIONS CONFORMING
 TO ASTM C 990 (TYP)



PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE

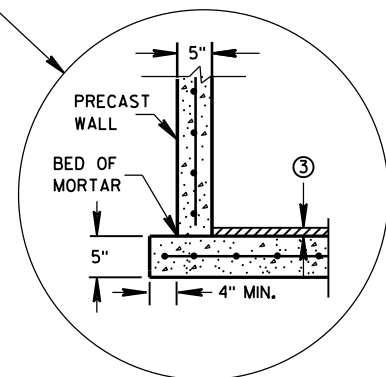
SECTION A-A



CAST-IN-PLACE REINFORCED CONCRETE

CONCRETE BLOCK ON CAST-IN-PLACE WITH PRECAST REINFORCED CONCRETE BASE ①

SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

CATCH BASINS 2X3-FT AND 2.5X3-FT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST CATCH BASIN UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

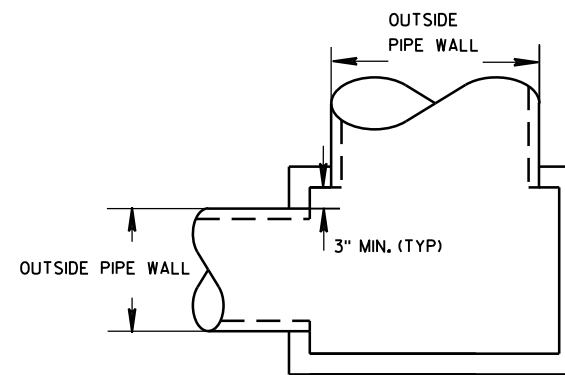
- ① FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.
- ③ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

CATCH BASIN COVER MATRIX

CATCH BASIN SIZE	WIDTH ① (FT)	LENGTH ② (FT)	F	ALL H'S
2X3-FT	2	3		X
2.5X3-FT	2.5	3	X	

PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	WIDTH (IN)	LENGTH (IN)
2X3-FT	12	24
2.5X3-FT	18	24

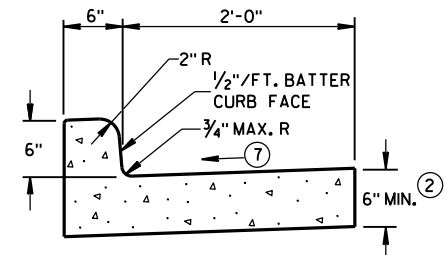


DETAIL "A"

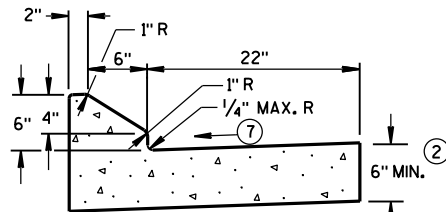
CATCH BASINS 2X3-FT
AND 2.5X3-FT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

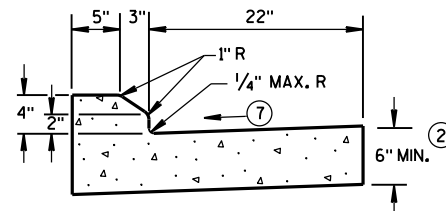
APPROVED
Sept., 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



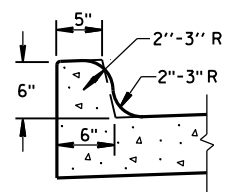
TYPES A^① & D



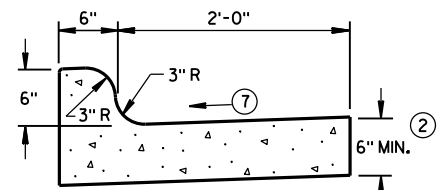
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

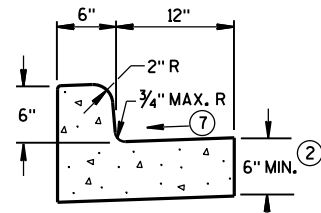


TYPES K^① & L
(OPTIONAL CURB SHAPE)



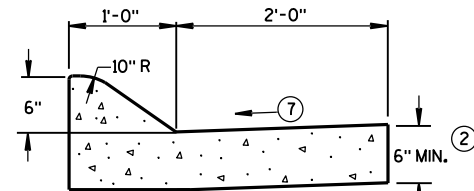
TYPES K^① & L

CONCRETE CURB & GUTTER 30"

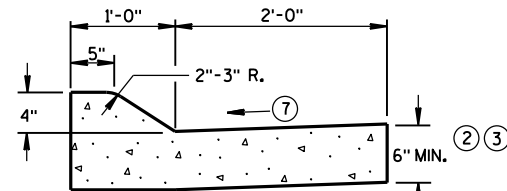


TYPES A^① & D

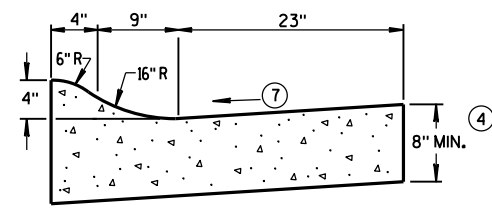
CONCRETE CURB & GUTTER 18"



6" SLOPED CURB TYPES A^① & D

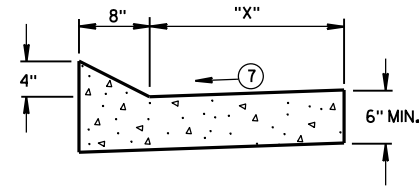


4" SLOPED CURB TYPES A^① & D



4" SLOPED CURB TYPES R^① & T^⑤

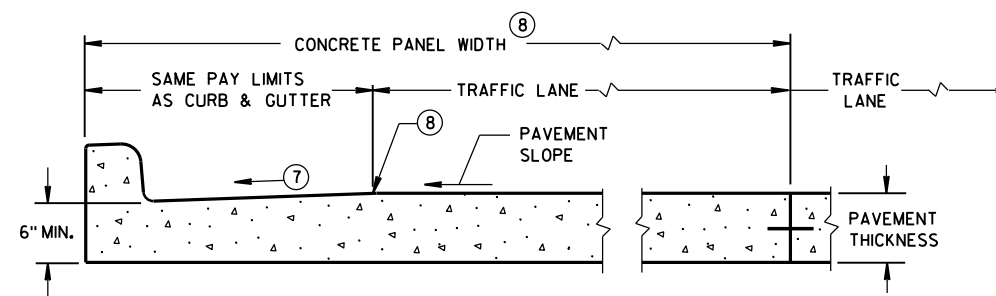
CONCRETE CURB & GUTTER 36"



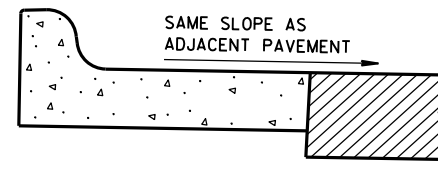
TYPES TBT & TBTT^①

CONCRETE CURB & GUTTER

TBT & TBTT	"X"
30"	22"
36"	28"



PARTIAL SECTION OF PAVEMENT
WITH INTEGRAL CURB & GUTTER



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

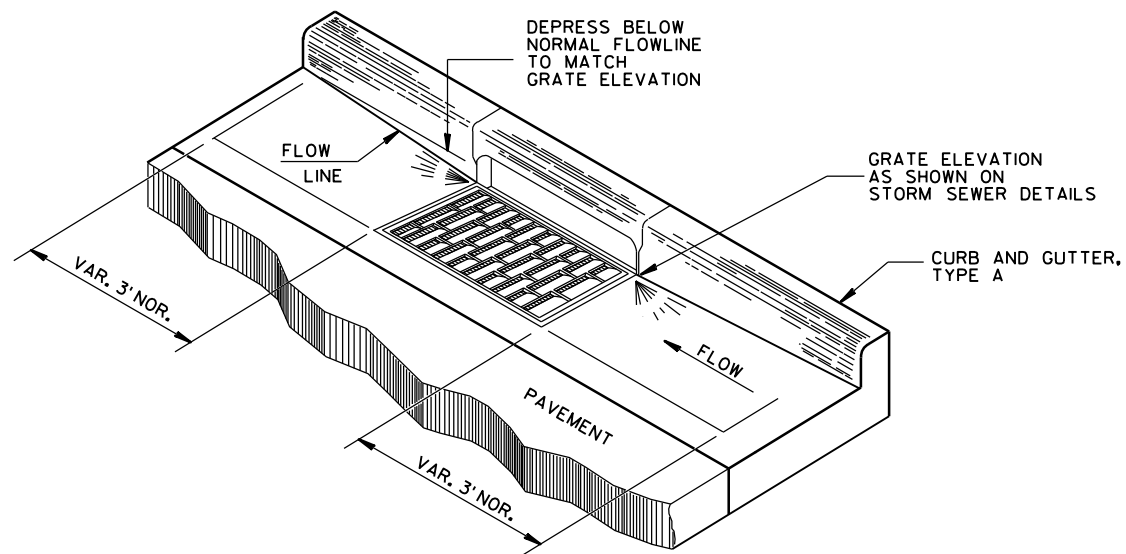
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'

* BIKE LANE IS NOT SHOWN.

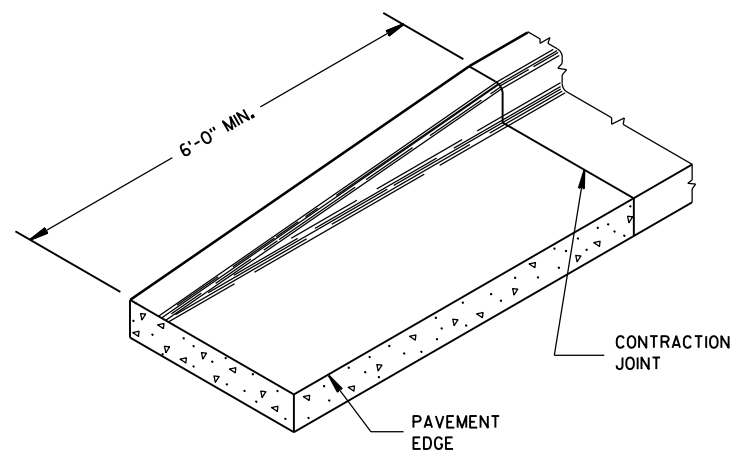
CONCRETE CURB & GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

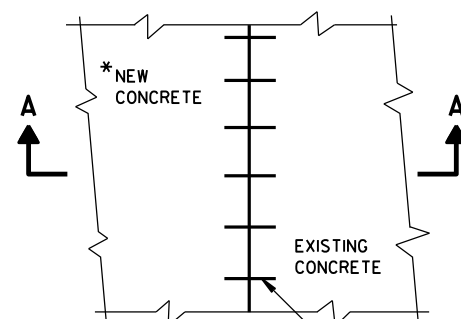


DETAIL OF CURB AND GUTTER AT INLETS

(TYPE H INLET COVER SHOWN)

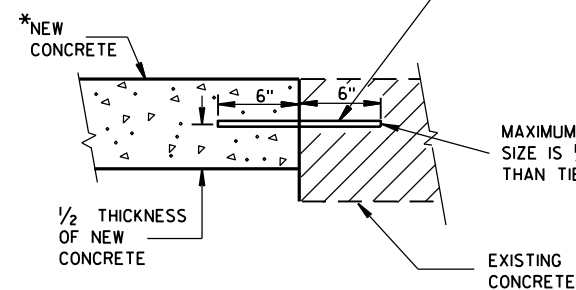


END SECTION CURB & GUTTER



PLAN VIEW

*NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.



**SECTION A-A
TIE BARS DRILLED
INTO EXISTING PAVEMENT**

NO. 6 TIE BARS SPACED 2'-6" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT.

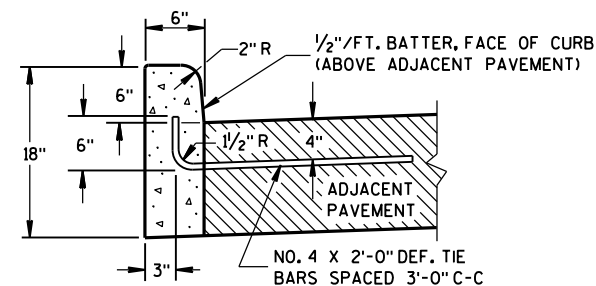
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

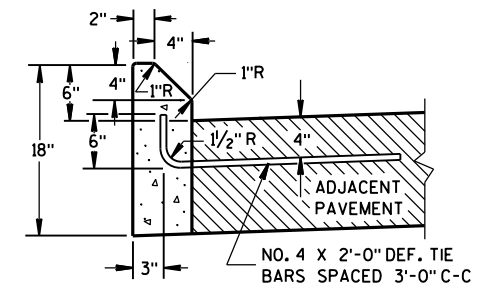
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 8D18 AND SDD 8D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

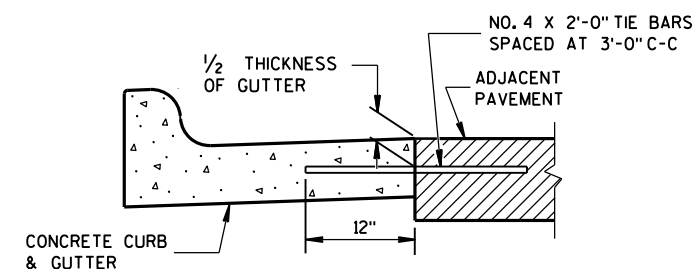


TYPES A^① & D

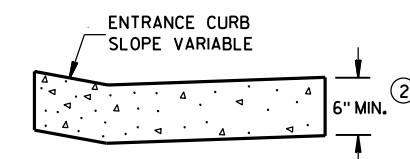


TYPES G^① & J

CONCRETE CURB



TYPICAL TIE BAR LOCATION^①



DRIVEWAY ENTRANCE CURB^⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June, 2017

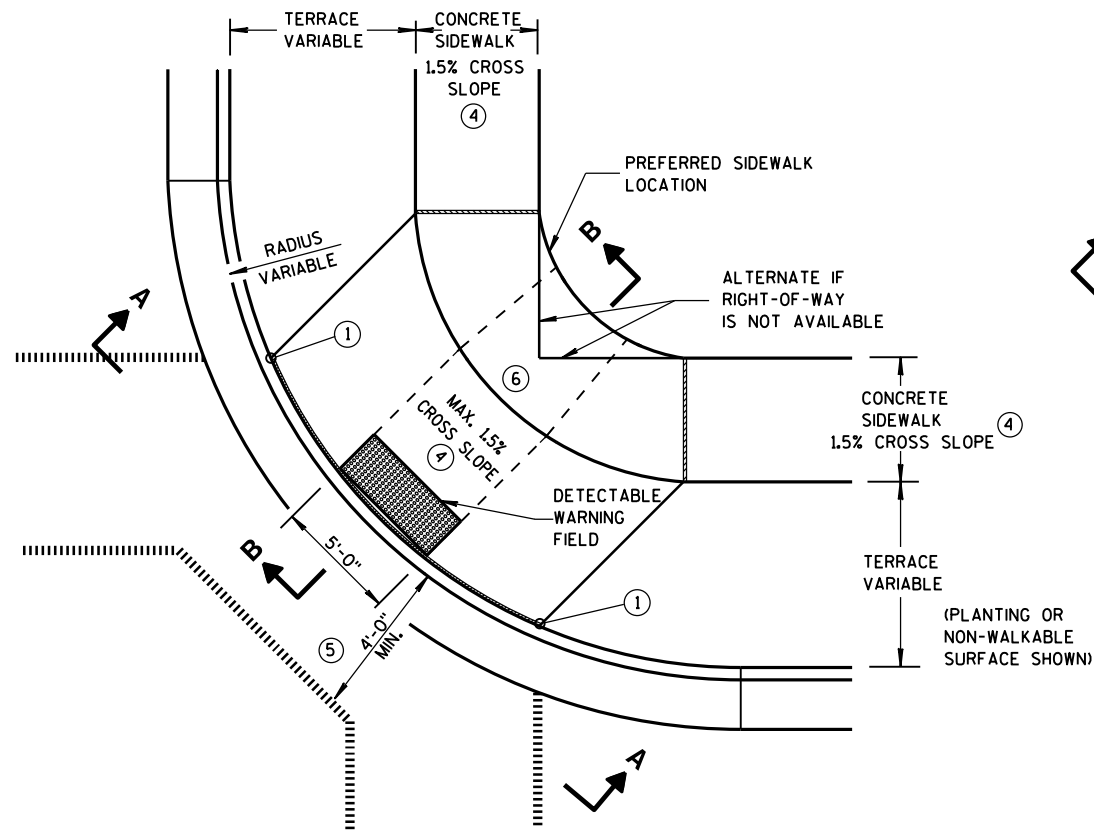
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FHWA

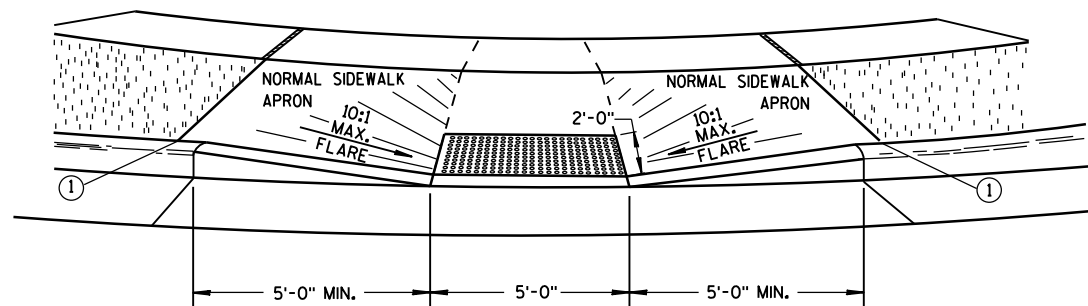
/S/ Rodney Taylor

ROADWAY STANDARDS DEVELOPMENT

UNIT SUPERVISOR

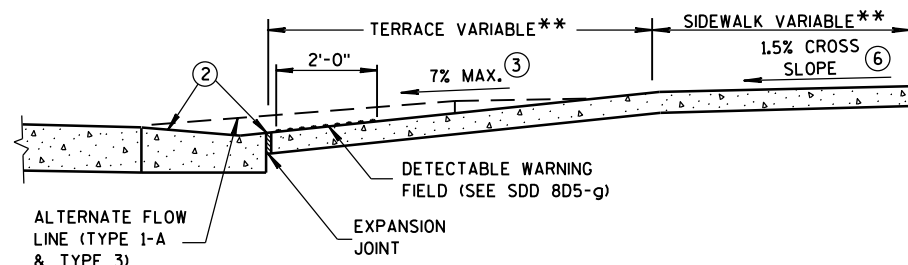


**PLAN VIEW
TYPE 1 RAMP**
(CENTER OF CORNER RADIUS)

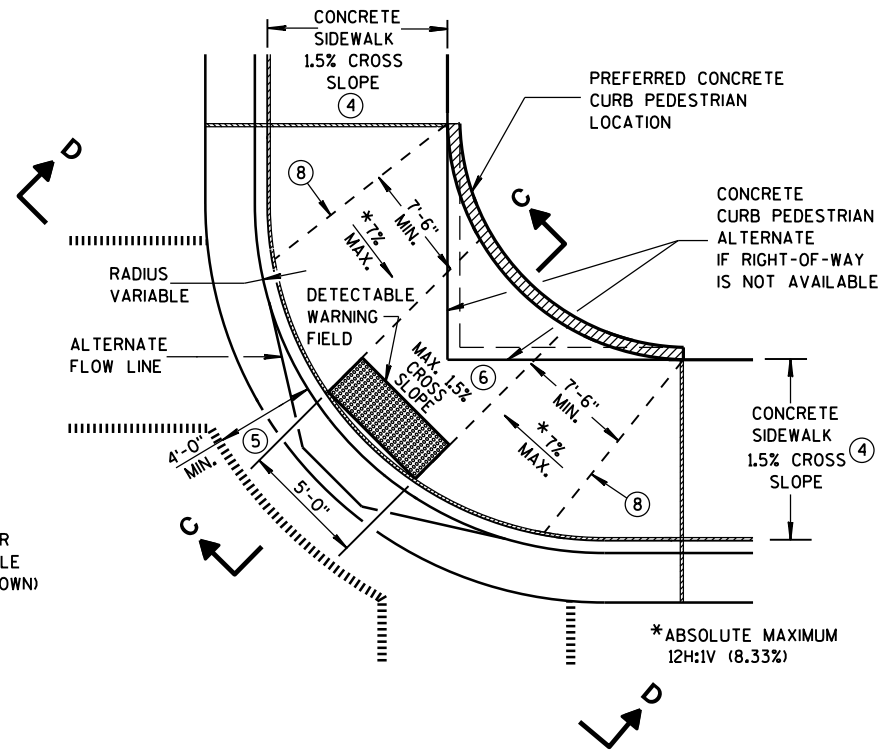


VIEW A-A

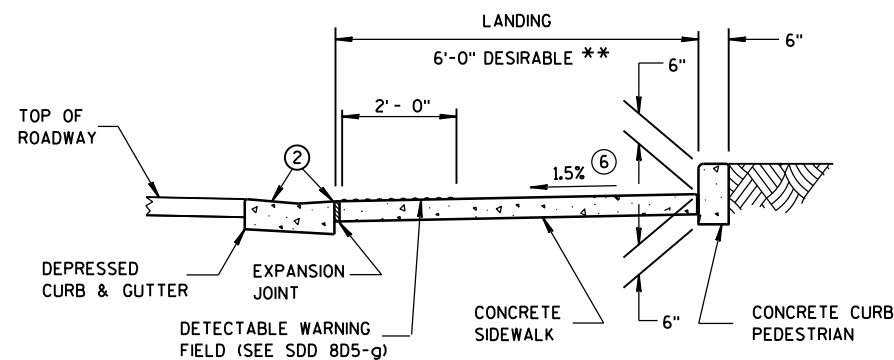
** WIDTH SHOWN ELSEWHERE
IN THE PLANS



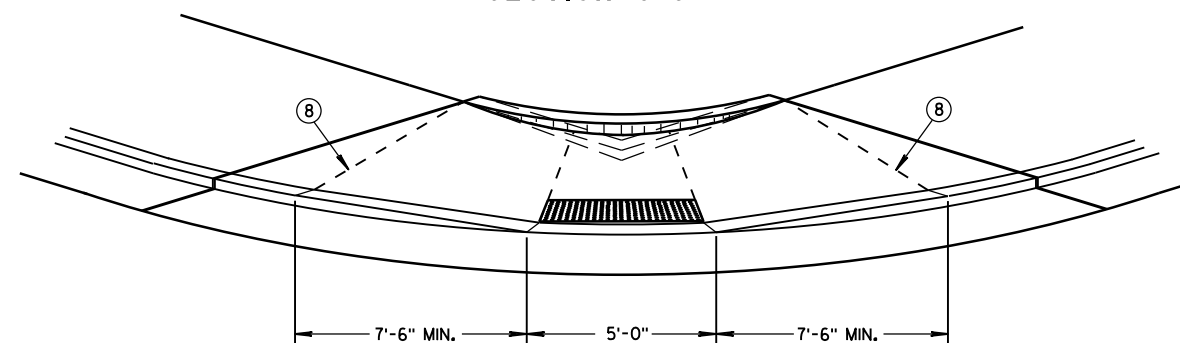
SECTION B-B



**PLAN VIEW
TYPE 1-A RAMP**
(NO TERRACE)



SECTION C-C



VIEW D-D

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

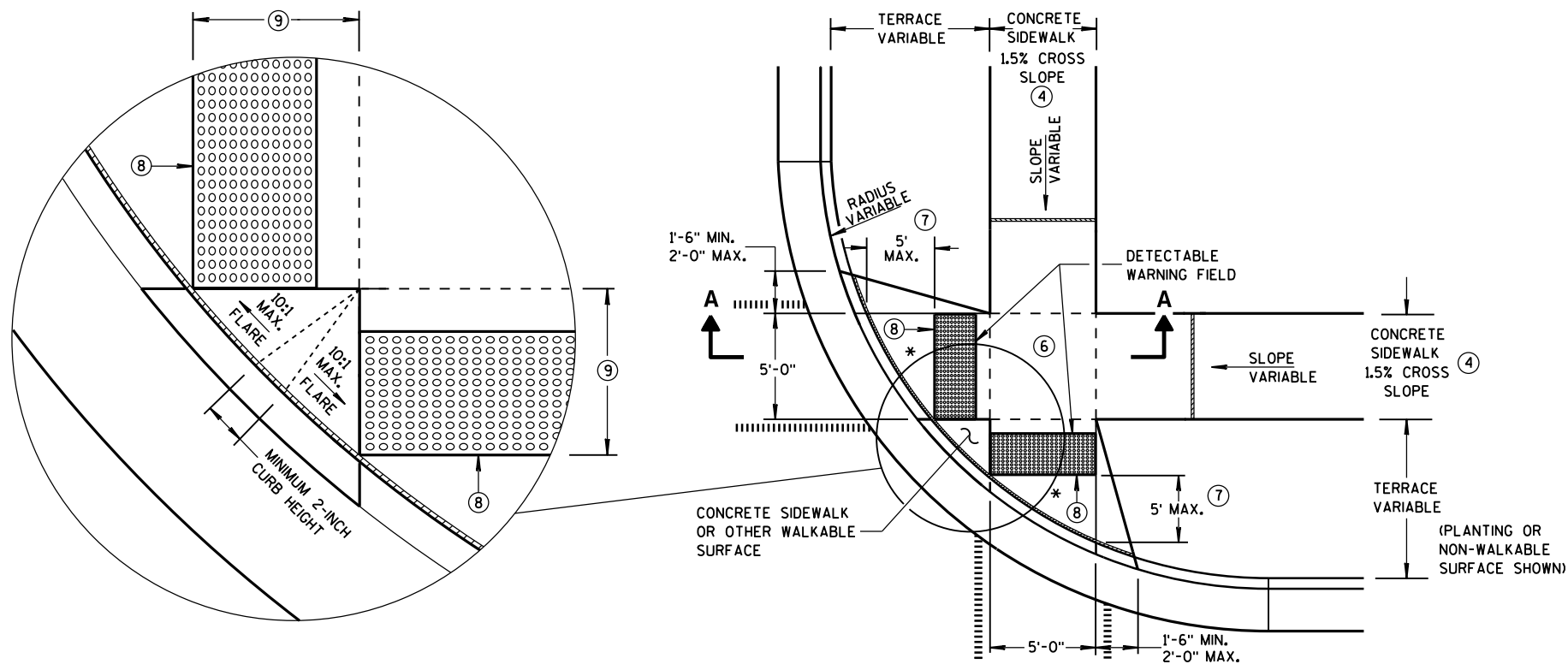
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ $\pm 0.5\%$ CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA. (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT

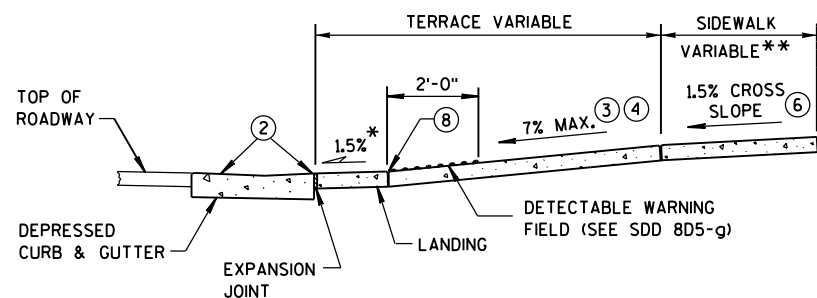
**CURB RAMPS
TYPES 1 AND 1-A**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



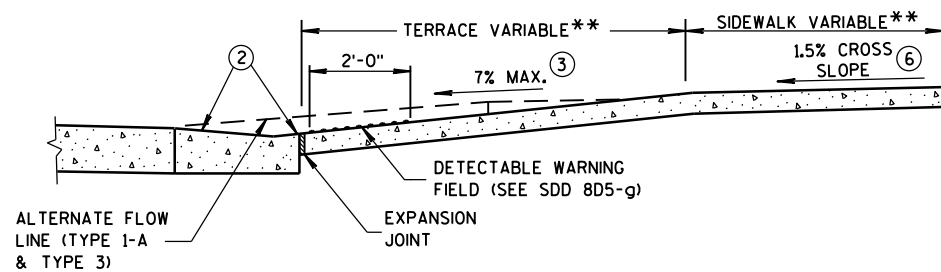
**PLAN VIEW
TYPE 2 RAMP**
(ON LINE WITH SIDEWALK)

* MAXIMUM 2.0% SLOPE
IN ALL DIRECTIONS IN
FRONT OF GRADE BREAK



SECTION A-A

** WIDTH SHOWN ELSEWHERE
IN THE PLANS



SECTION B-B

GENERAL NOTES

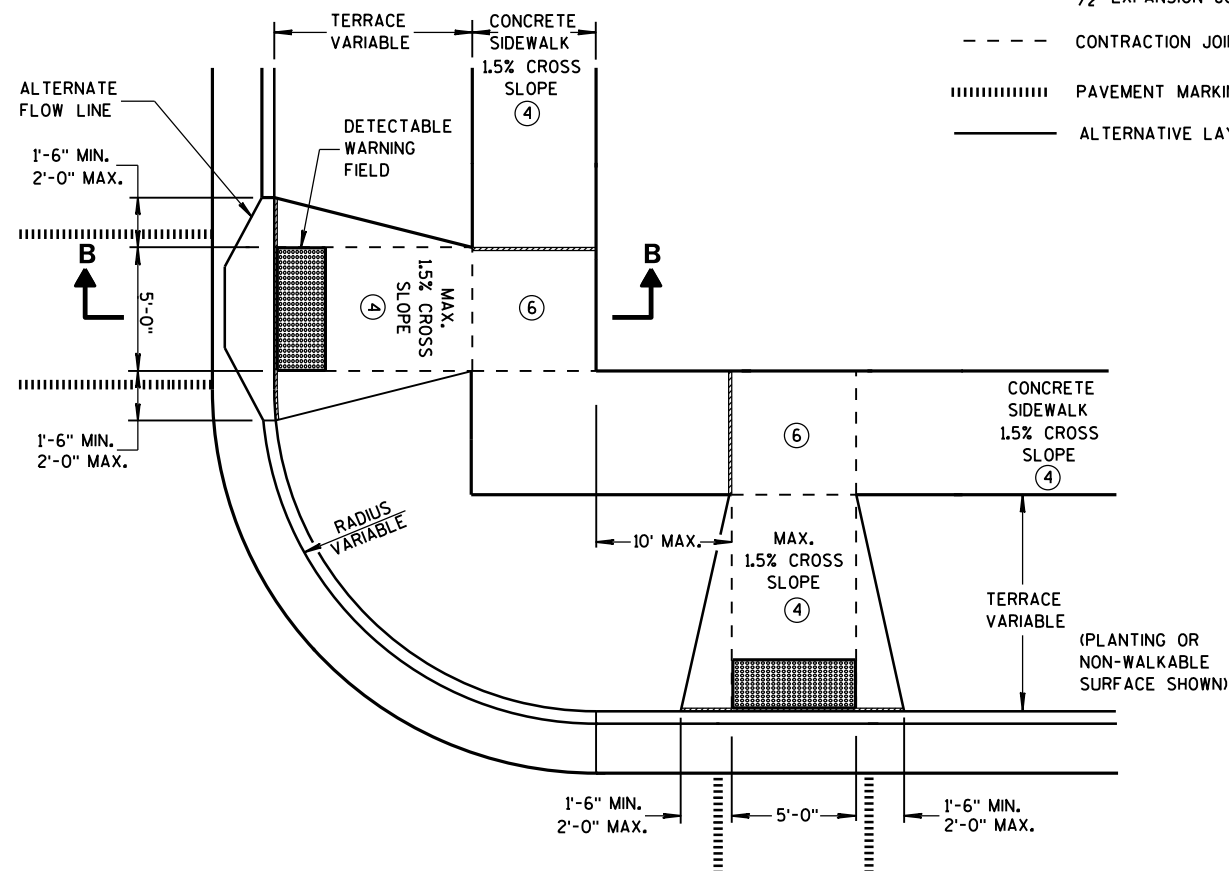
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN THIS DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

LEGEND

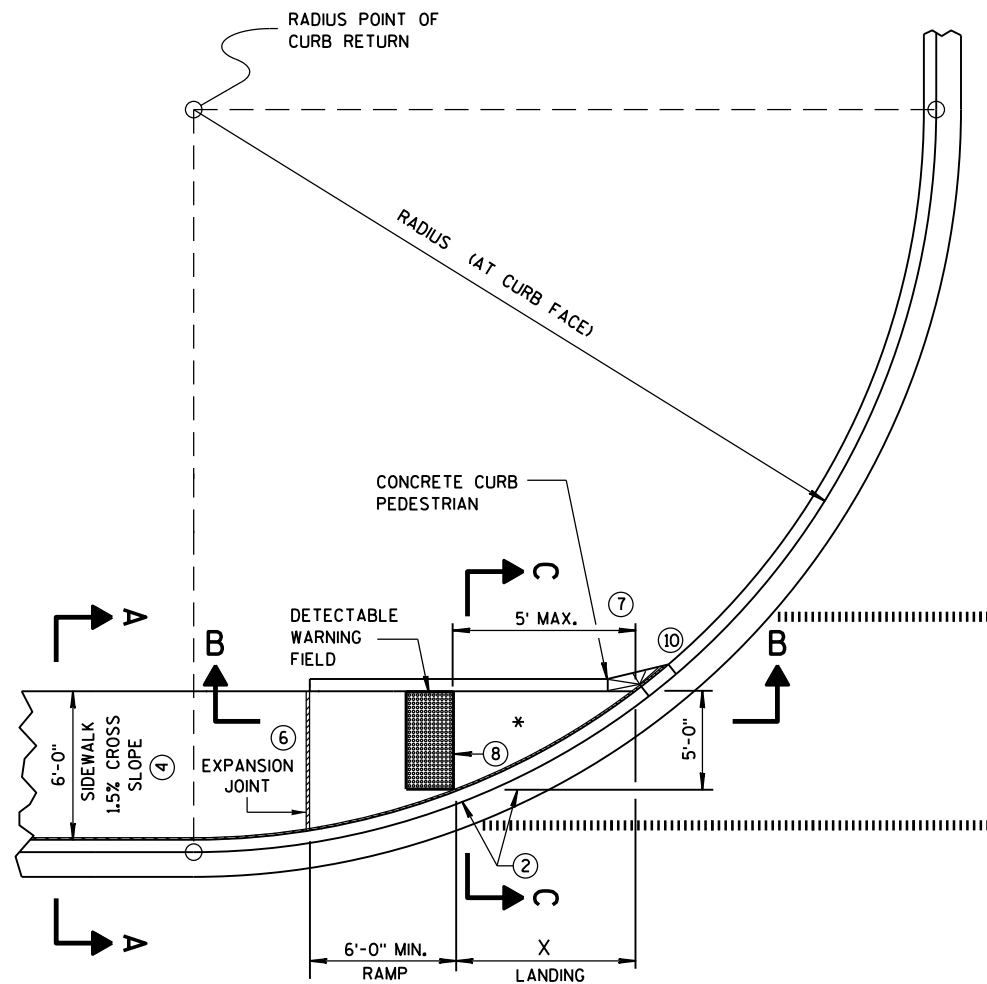
- 1/2" EXPANSION JOINT-SIDEWALK
- - - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



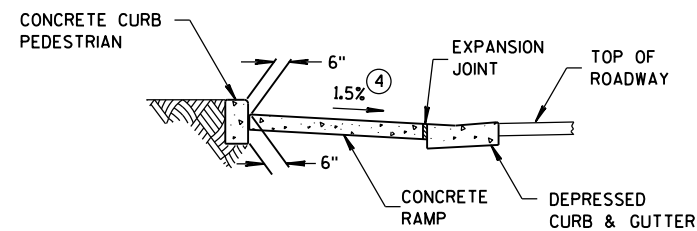
**PLAN VIEW
TYPE 3 RAMP**
(OUTSIDE OF CROSSWALK AREA)

**CURB RAMPS
TYPES 2 AND 3**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

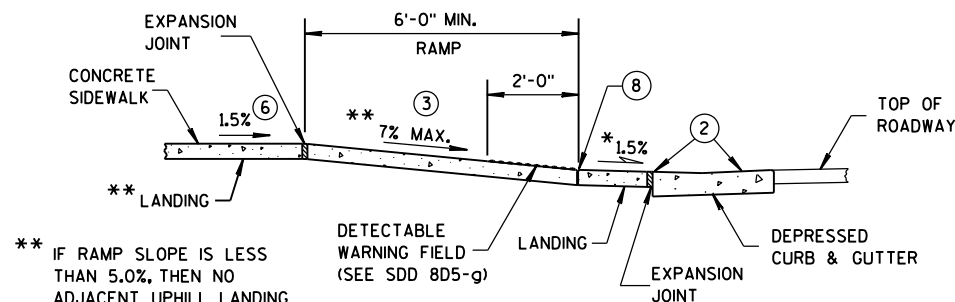


CURB RAMP TYPE 4A
PLAN VIEW



SECTION C-C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE
IN ALL DIRECTIONS IN
FRONT OF GRADE BREAK

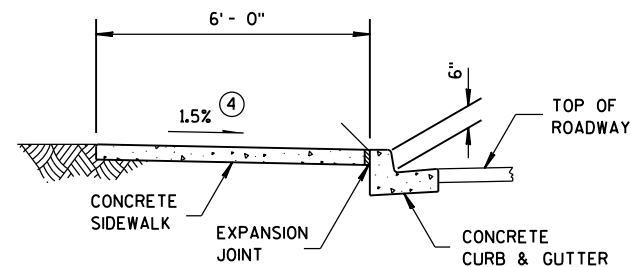


SECTION B-B FOR TYPE 4A

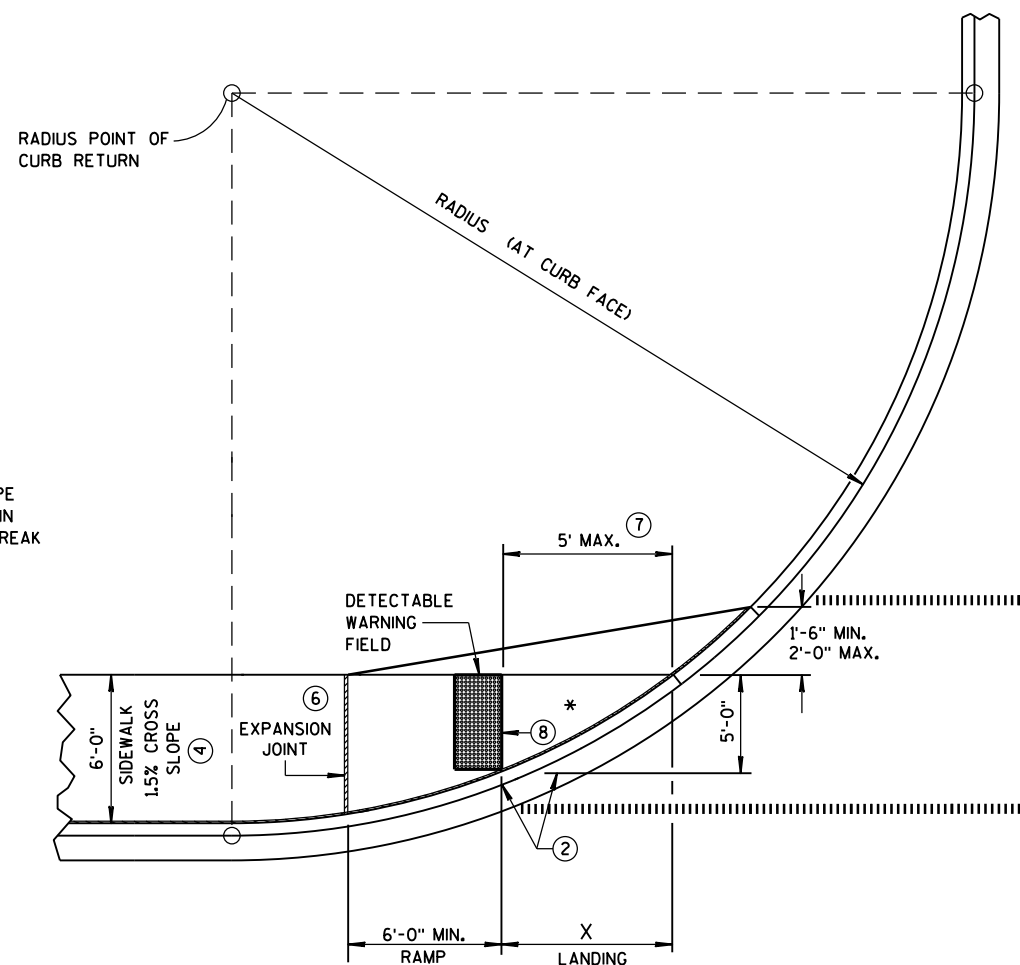
** IF RAMP SLOPE IS LESS
THAN 5.0%, THEN NO
ADJACENT UPHILL LANDING
IS REQUIRED

RADIUS (AT CURB FACE)	X
10 FEET	4'-7"
15 FEET	6'-5½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A



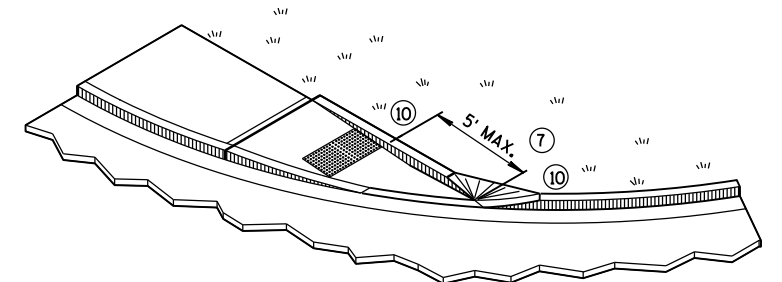
CURB RAMP TYPE 4A1
PLAN VIEW

GENERAL NOTES

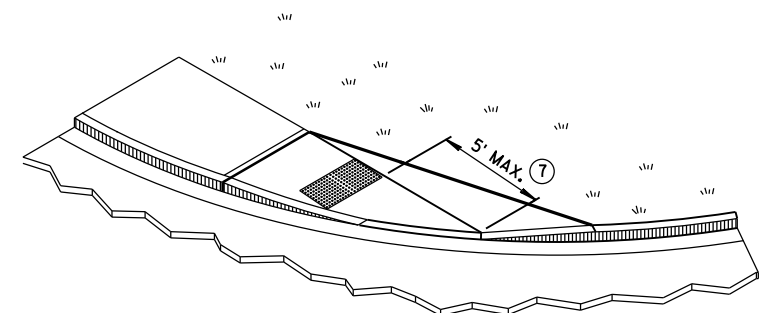
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



ISOMETRIC VIEW FOR TYPE 4A



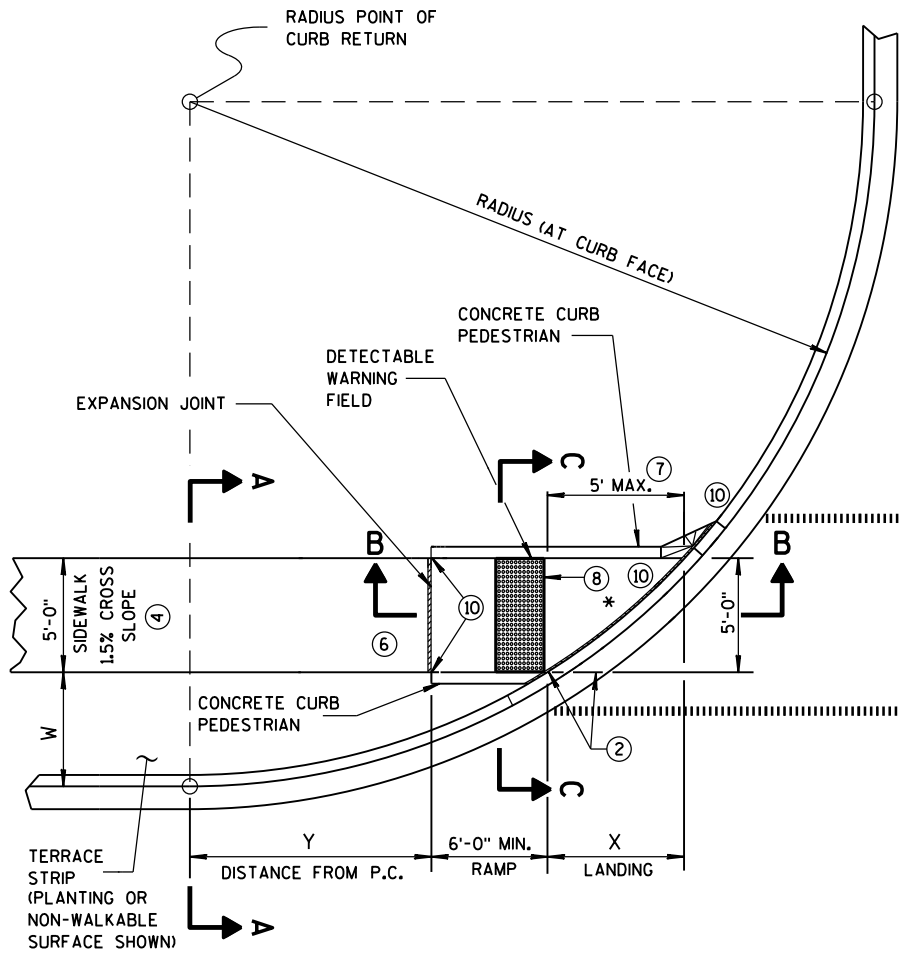
ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

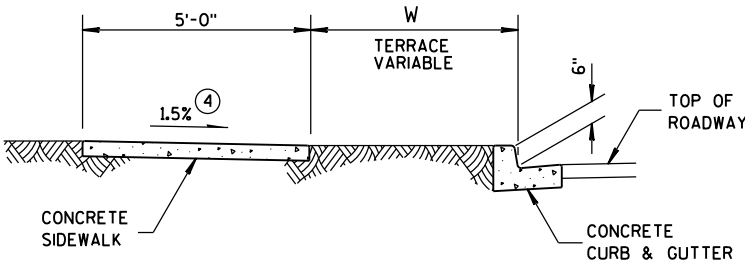
- ½" EXPANSION JOINT-SIDEWALK
- - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 4A AND 4A1

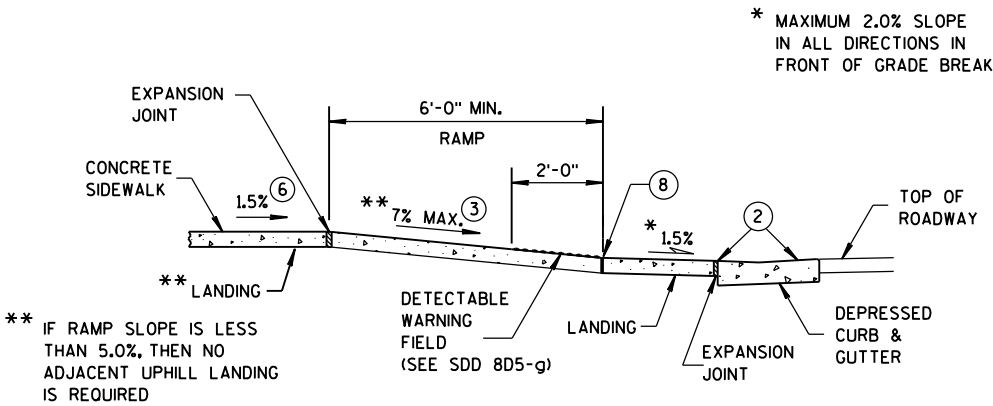
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**CURB RAMP TYPE 4B
PLAN VIEW**



SECTION A-A FOR TYPE 4B



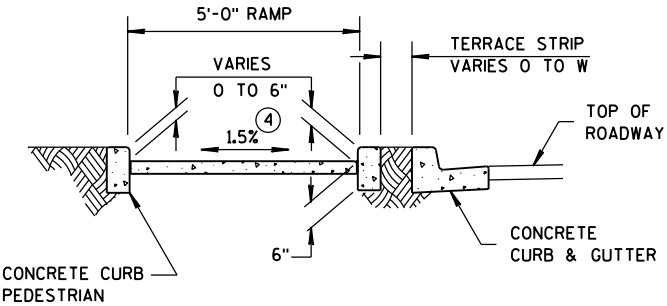
SECTION B-B FOR TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2'-10 1/4"	0'-5"	2'-1"	1'-4 1/2"	1'-5"	2'-1"	0'-10"	2'-7 1/2"	0'-3 1/4"	3'-0 1/4"						
15 FEET	4'-6 3/4"	2'-1 3/4"	3'-9"	3'-5 1/4"	3'-1 1/4"	4'-6"	2'-6 3/4"	5'-4 1/2"	2'-1"	6'-1"	1'-8"	6'-8 1/2"	1'-3 1/4"	7'-2 1/2"	0'-10 3/4"	7'-7 1/4"
20 FEET	5'-9 3/4"	3'-6 1/2"	4'-11 1/2"	5'-1 3/4"	4'-3 1/4"	6'-5 1/2"	3'-8 3/4"	7'-7"	3'-3"	8'-6 1/2"	2'-10"	9'-4 1/2"	2'-5 1/2"	10'-1 1/4"	2'-1 1/4"	10'-9"
30 FEET			6'-9 1/4"	7'-11 1/4"	6'-0 1/4"	9'-8"	5'-5"	11'-1 3/4"	4'-10 3/4"	12'-5 3/4"	4'-5 1/2"	13'-7 3/4"	4'-0 3/4"	14'-8 1/2"	3'-8 1/2"	15'-8 1/4"
40 FEET									6'-1 3/4"	15'-8 1/2"	5'-8"	17'-2"	5'-3"	18'-5 3/4"	4'-10 3/4"	19'-8 1/4"
50 FEET															5'-10 1/4"	23'-2"

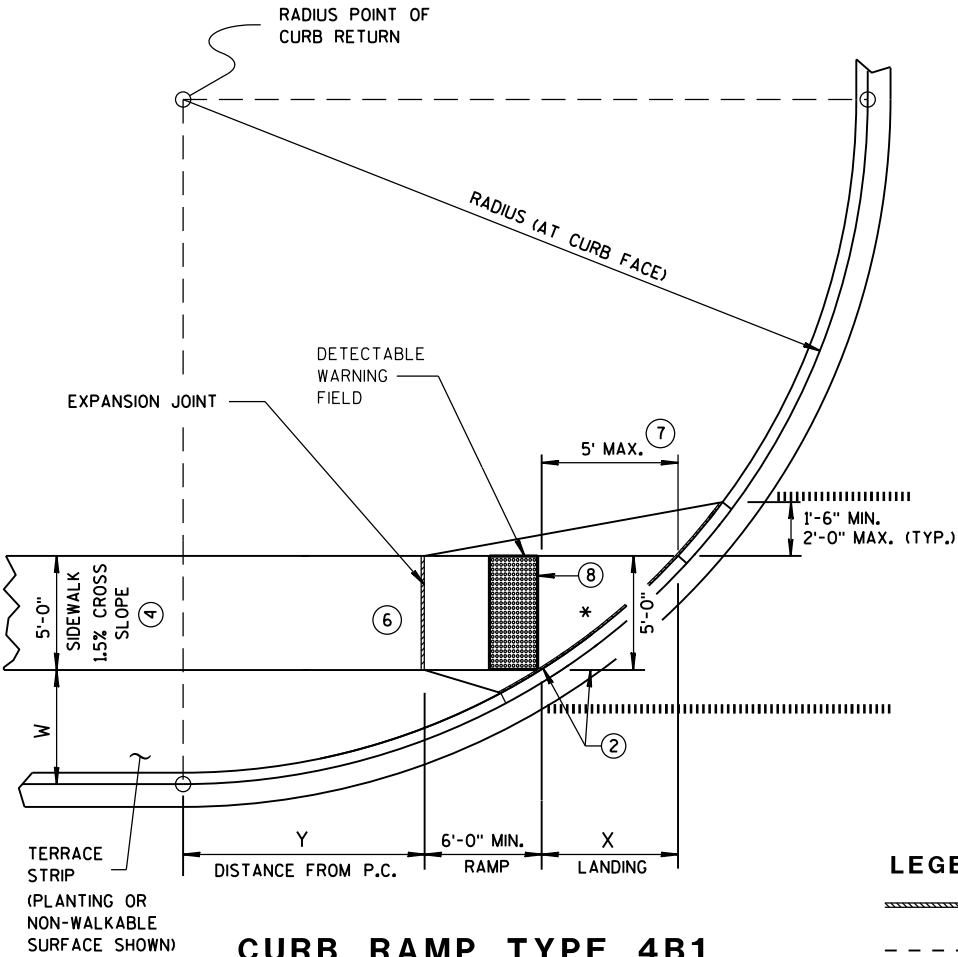
INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

GENERAL NOTES

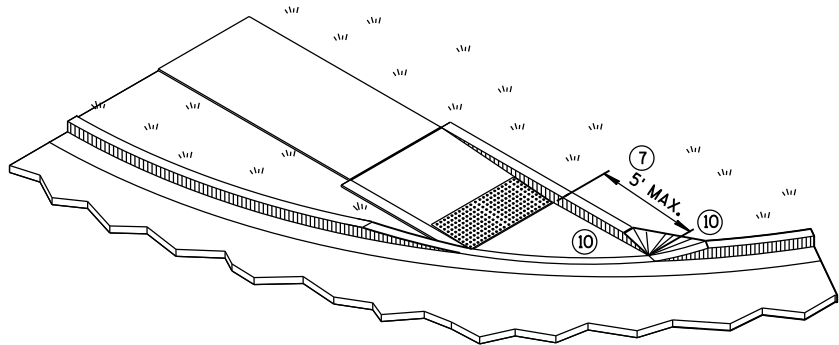
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



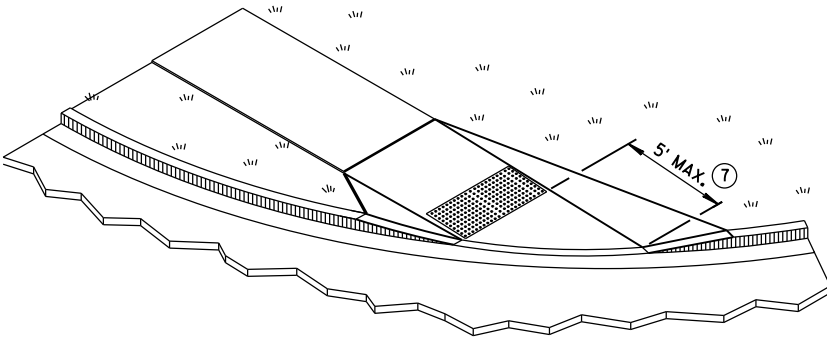
SECTION C-C FOR TYPE 4B



**CURB RAMP TYPE 4B1
PLAN VIEW**



ISOMETRIC VIEW FOR TYPE 4B



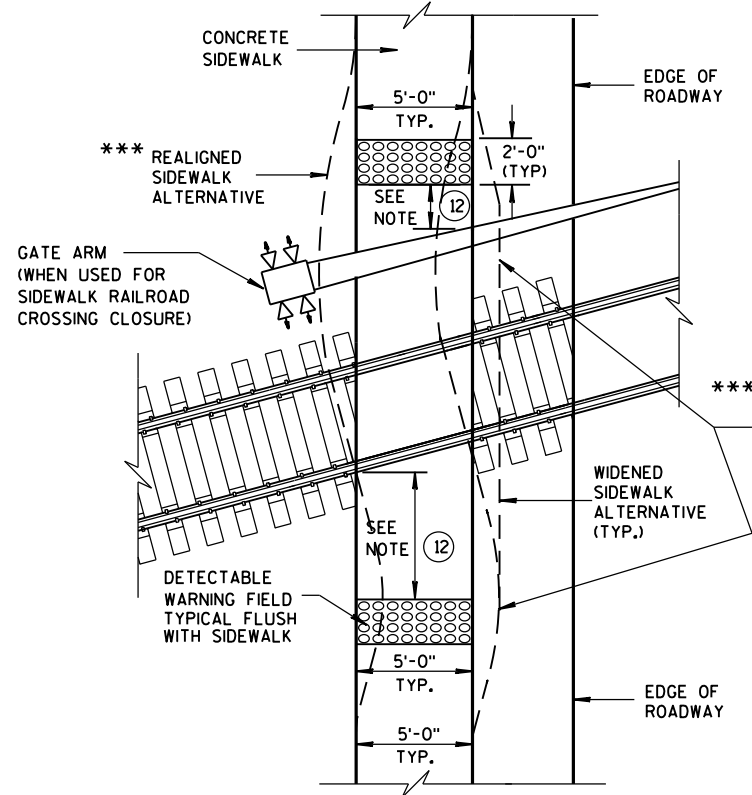
ISOMETRIC VIEW FOR TYPE 4B1

LEGEND

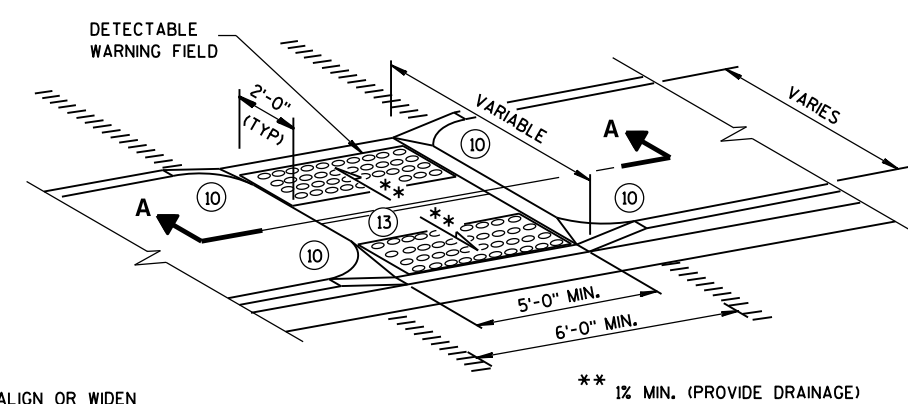
- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 4B AND 4B1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



TYPE 8
DETECTABLE WARNINGS
AT RAILROAD CROSSING

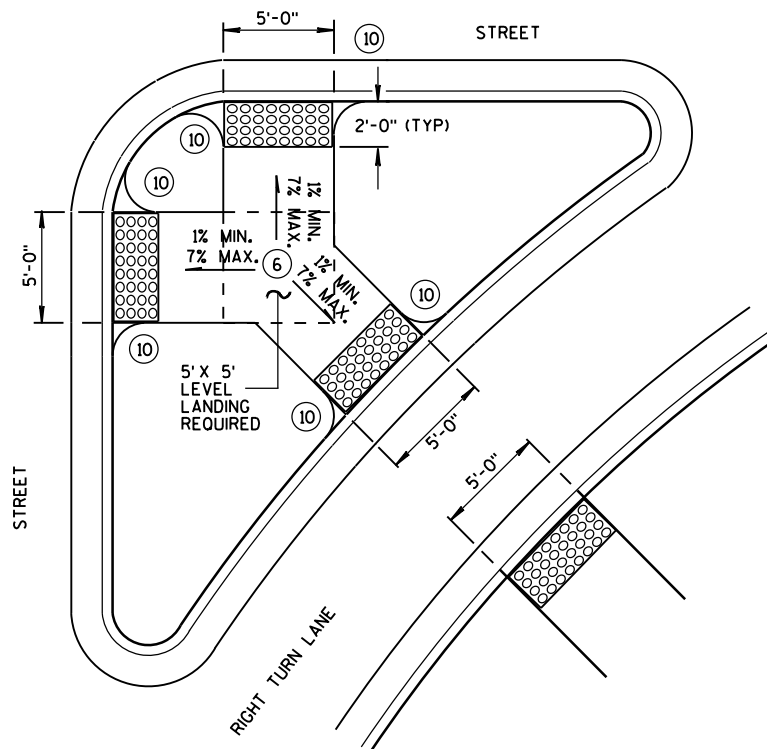


MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING
TYPE 5

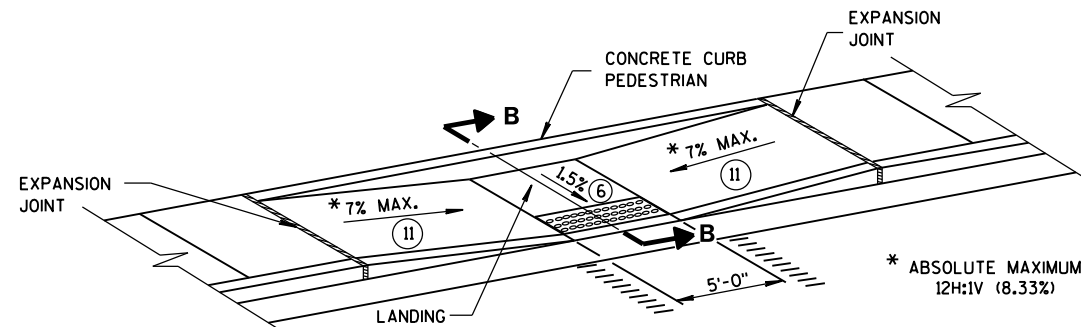
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑩ INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 15 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2-FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

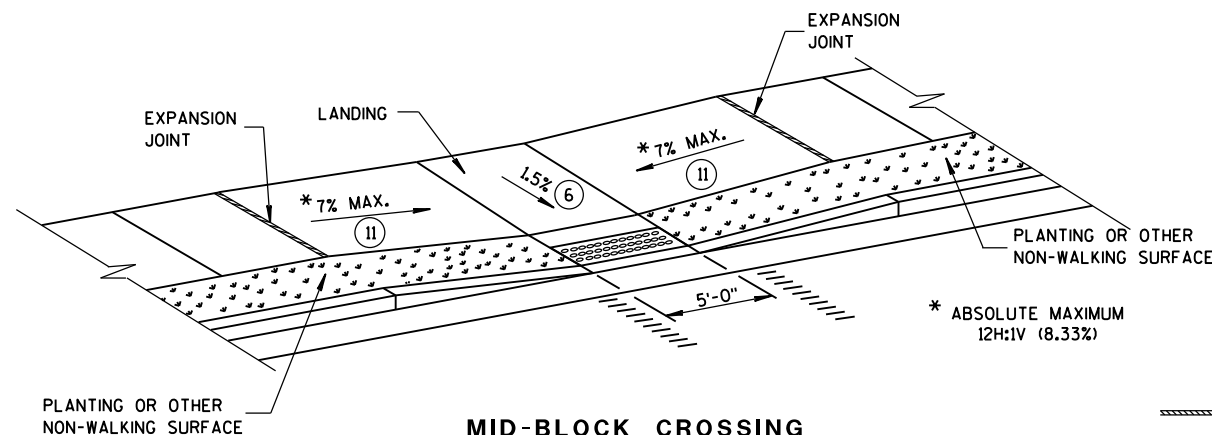
REFER TO GENERAL NOTES ② AND ③
FOR ALL ISLAND CURB RAMP



TYPE 6
DETECTABLE WARNING AT ISLANDS

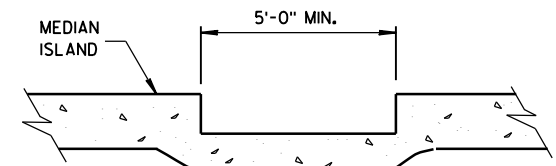


MID-BLOCK CROSSING
TYPE 7A

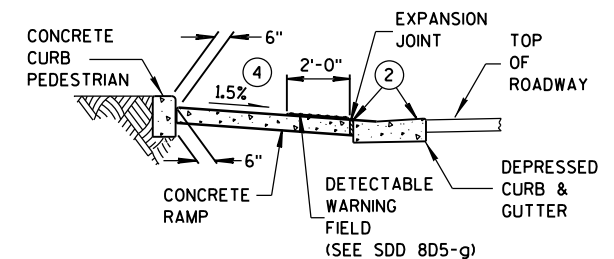


MID-BLOCK CROSSING
TYPE 7B

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMP
MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.



SECTION A-A



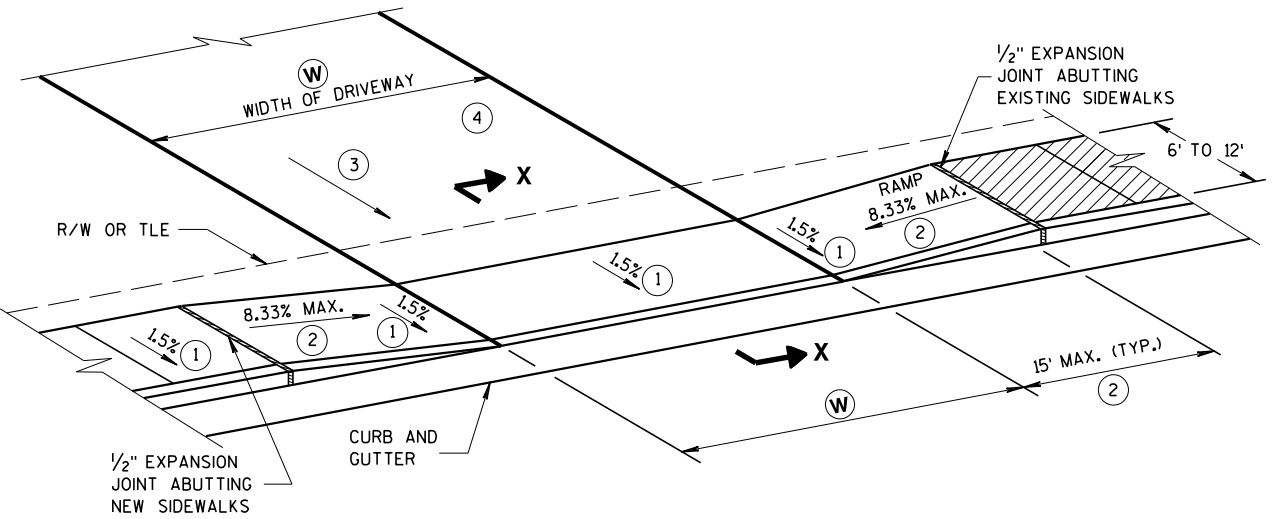
SECTION B-B

LEGEND

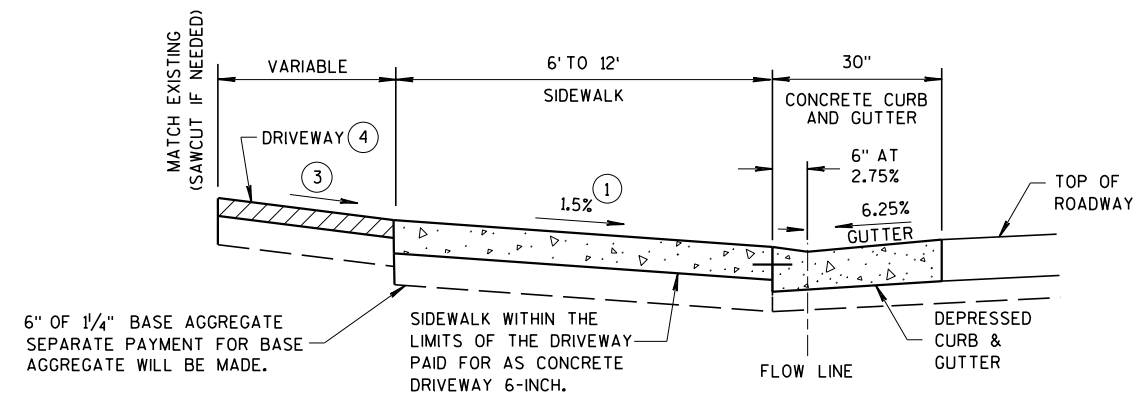
- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMP
TYPES 5, 6, 7A, 7B & 8

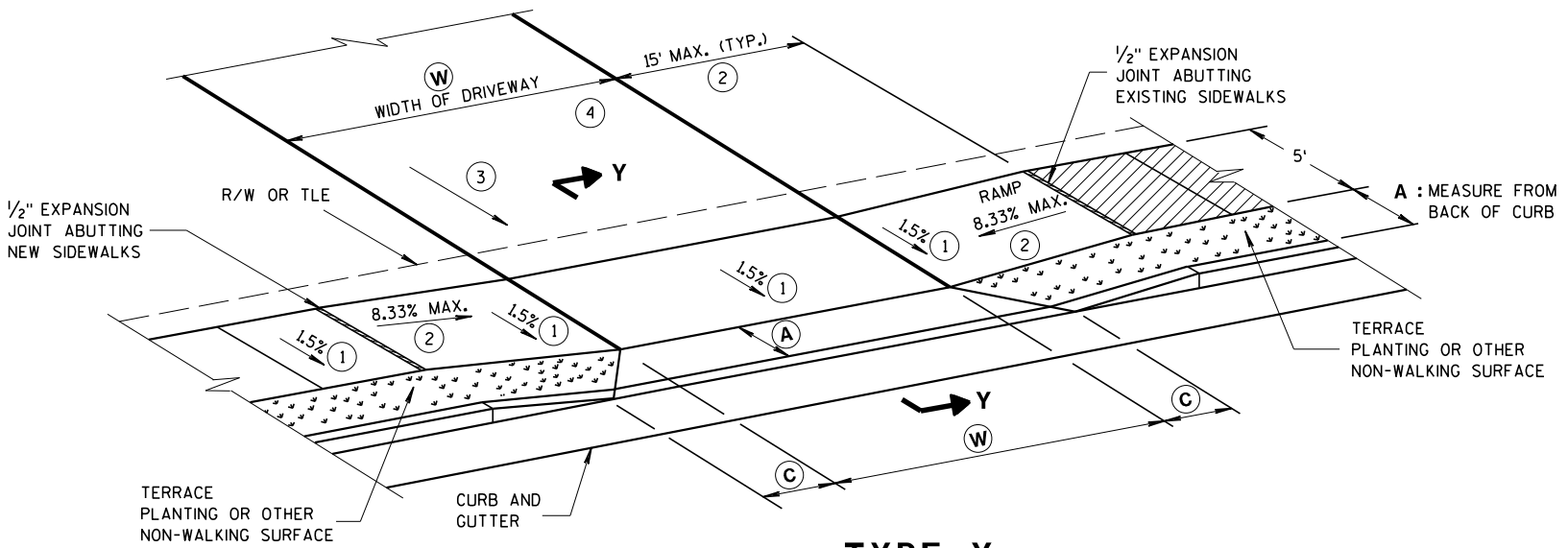
STATE OF WISCONSIN
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TYPE X
SIDEWALK ABUTS CURB & GUTTER
TERRACE VARIES 0 TO 3 FEET

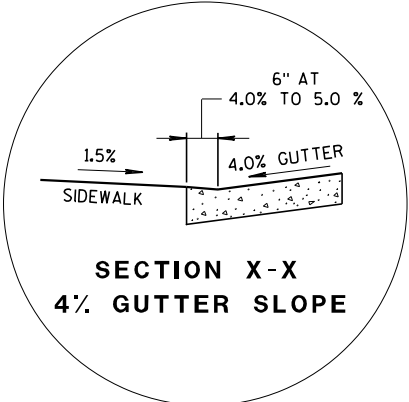


SECTION X-X

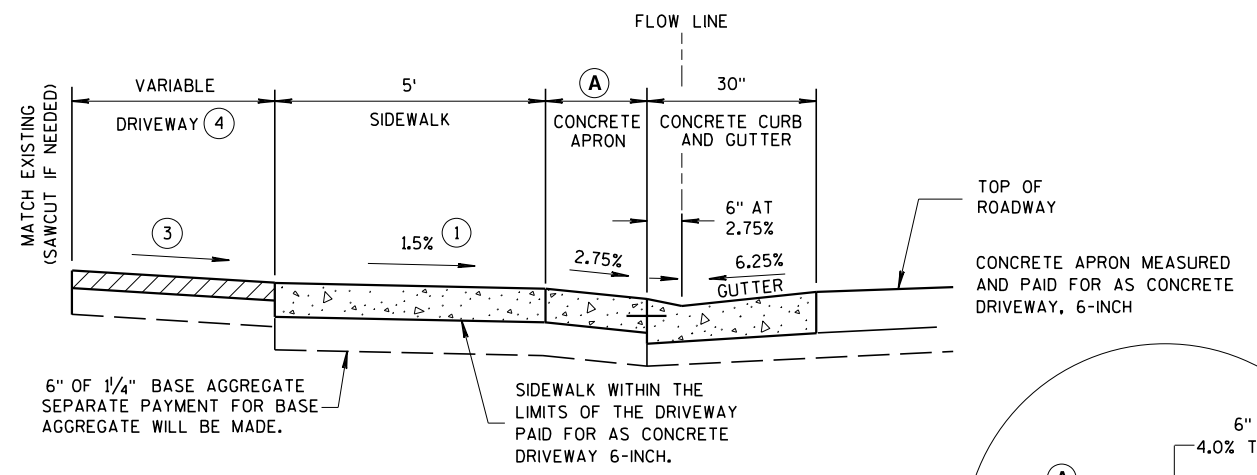


TYPE Y
SIDEWALK WITH NARROWER TERRACE
TERRACE VARIES 4 TO 6 FEET

W: 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



SECTION X-X
4% GUTTER SLOPE

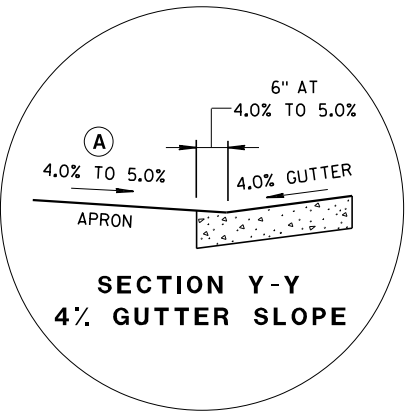


SECTION Y-Y
DRIVEWAY DETAIL
WITH CONCRETE CURB & GUTTER
(URBAN AND SUBURBAN)

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS

TABLE Y

(A) FEET	(C) FEET
3.5'	2.0'
4.5'	3.0'
5.5'	3.5'



SECTION Y-Y
4% GUTTER SLOPE

GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- CONSTRUCTION TOLERANCE OF 0.5% ± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- DRIVEWAY SLOPES: DESIRABLE MAXIMUM**
10.5% UP AWAY FROM SIDEWALK (SAG)
8.5% DOWN AWAY FROM SIDEWALK (CREST)
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- DRIVEWAY TYPES**
 - 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
 - 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
 - 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES)

NOT TO SCALE

**DRIVEWAY AND SIDEWALK
RAMPS
TYPES X & Y**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

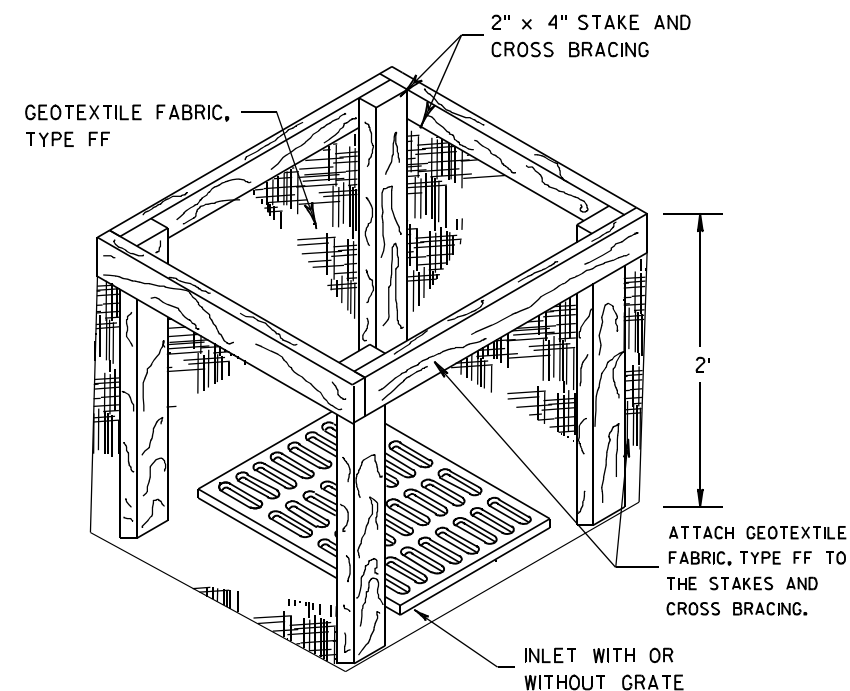
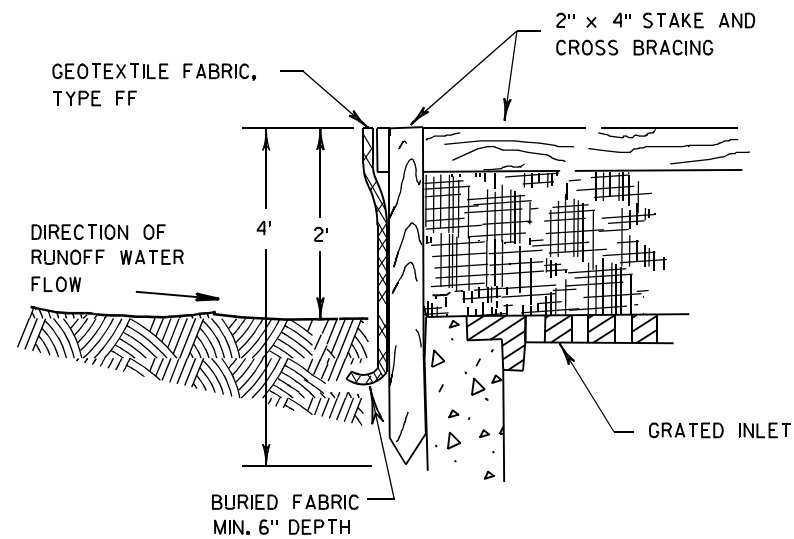
APPROVED
March 2018 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1½" X 1½" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



<div style="text-align: center;">SILT FENCE</div>	
<div style="text-align: center;">STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</div>	
<div>APPROVED <u>4-29-05</u> DATE</div>	<div><u>/S/ Beth Cannestra</u> CHIEF ROADWAY DEVELOPMENT ENGINEER</div>



INLET PROTECTION, TYPE A

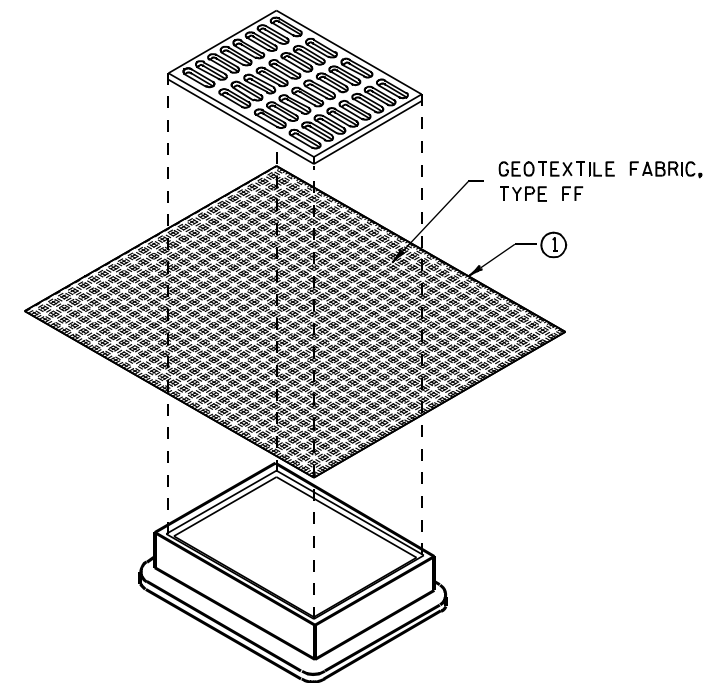
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE
DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE
SUBSTITUTED.

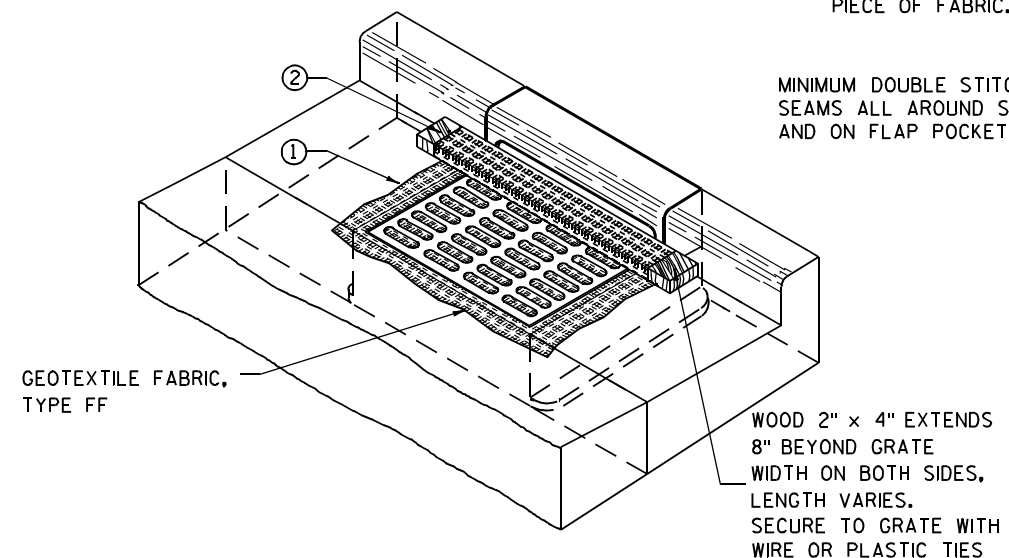
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



INLET PROTECTION, TYPE B (WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

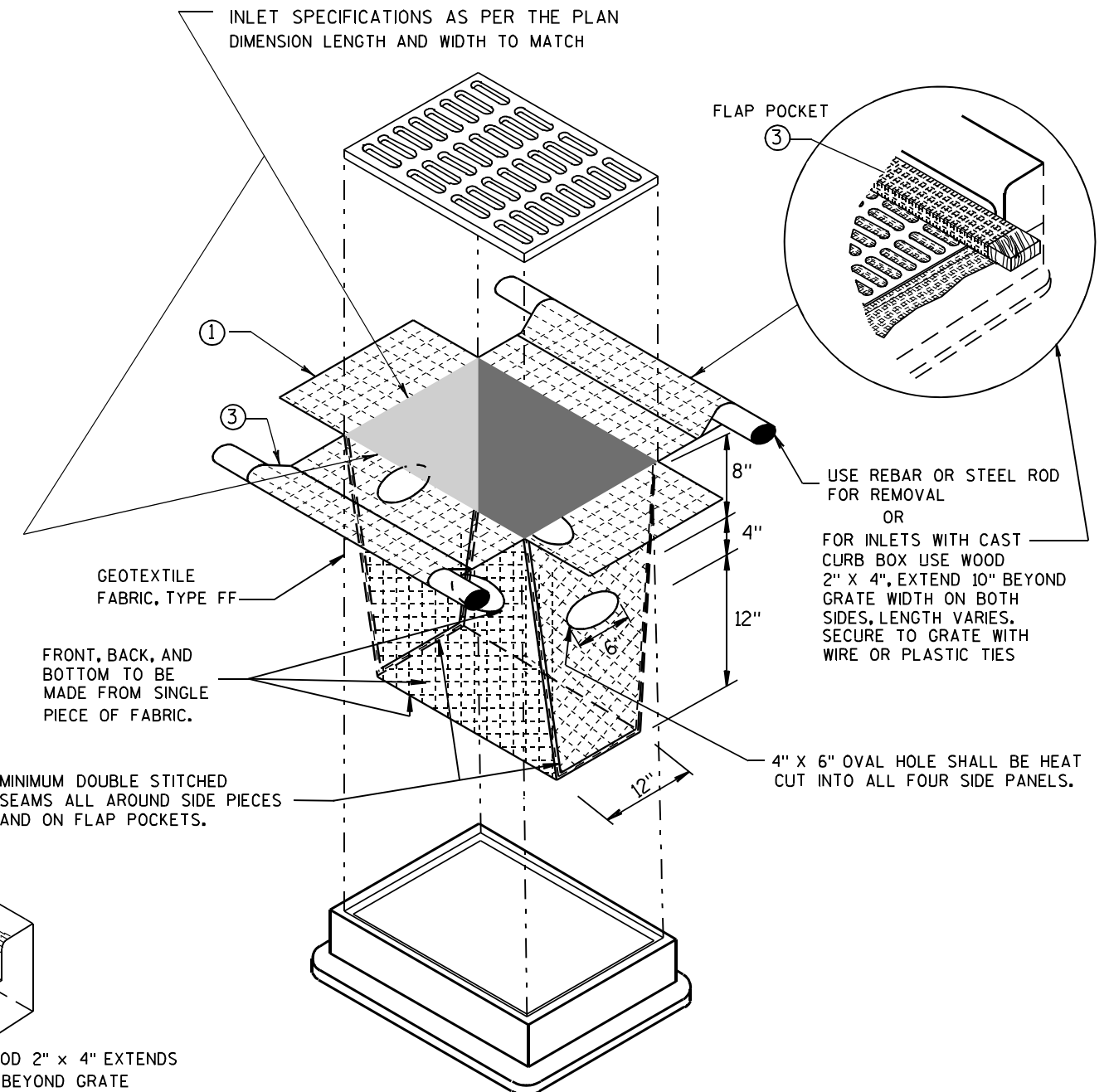
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE (2))

INLET PROTECTION TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

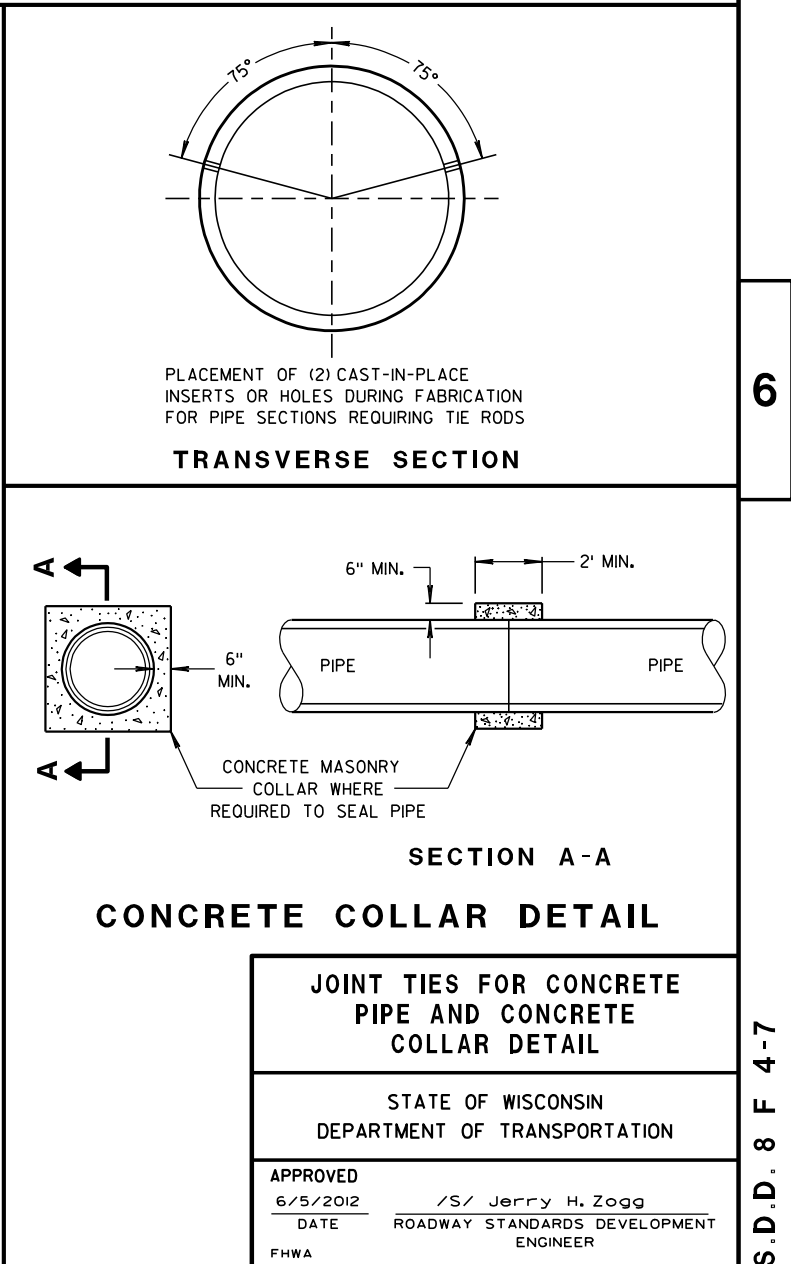
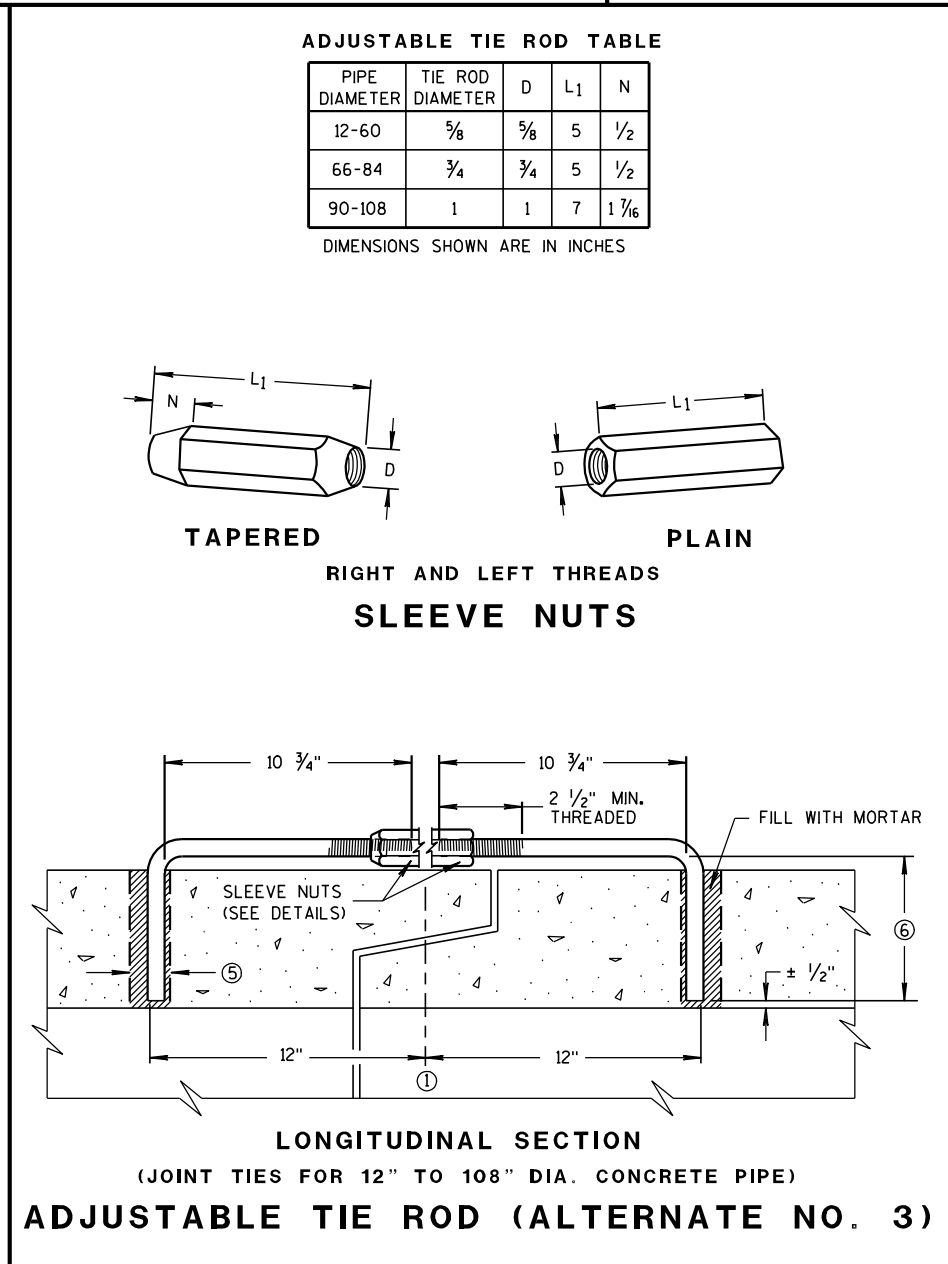
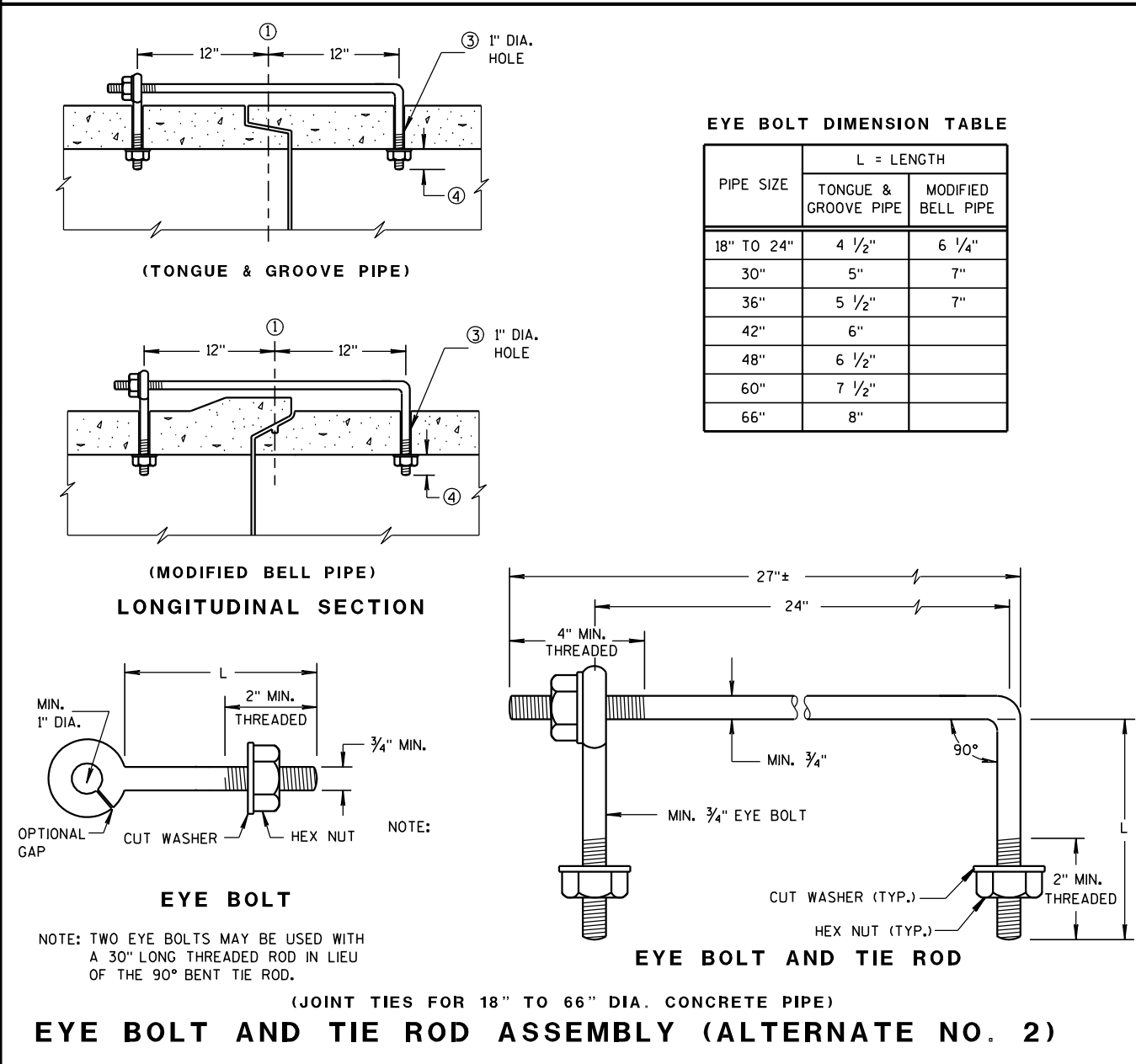
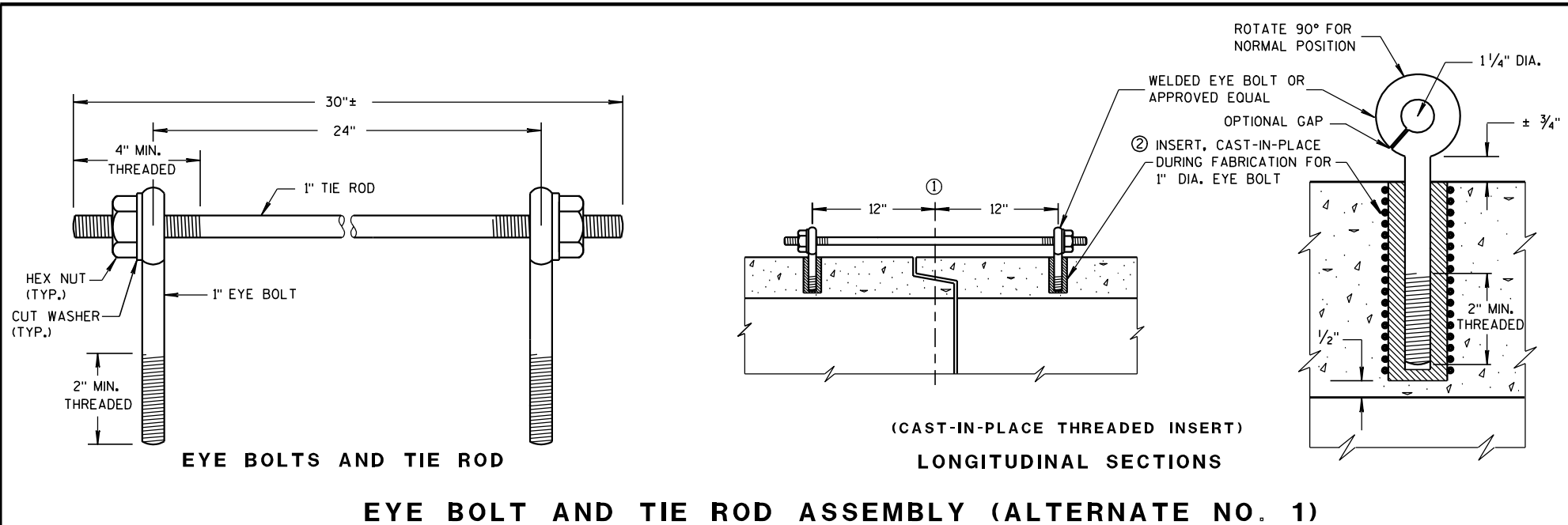
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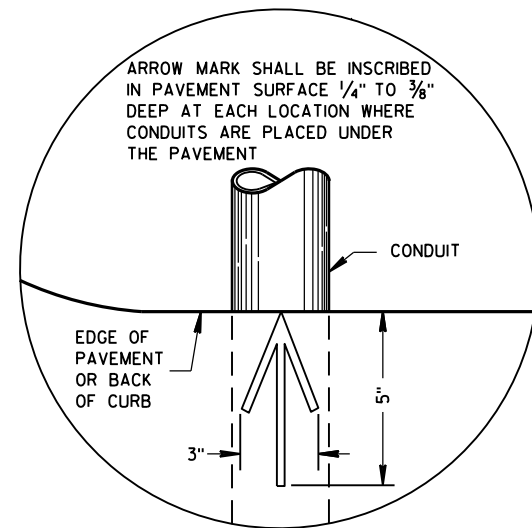
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DATE

FHWA

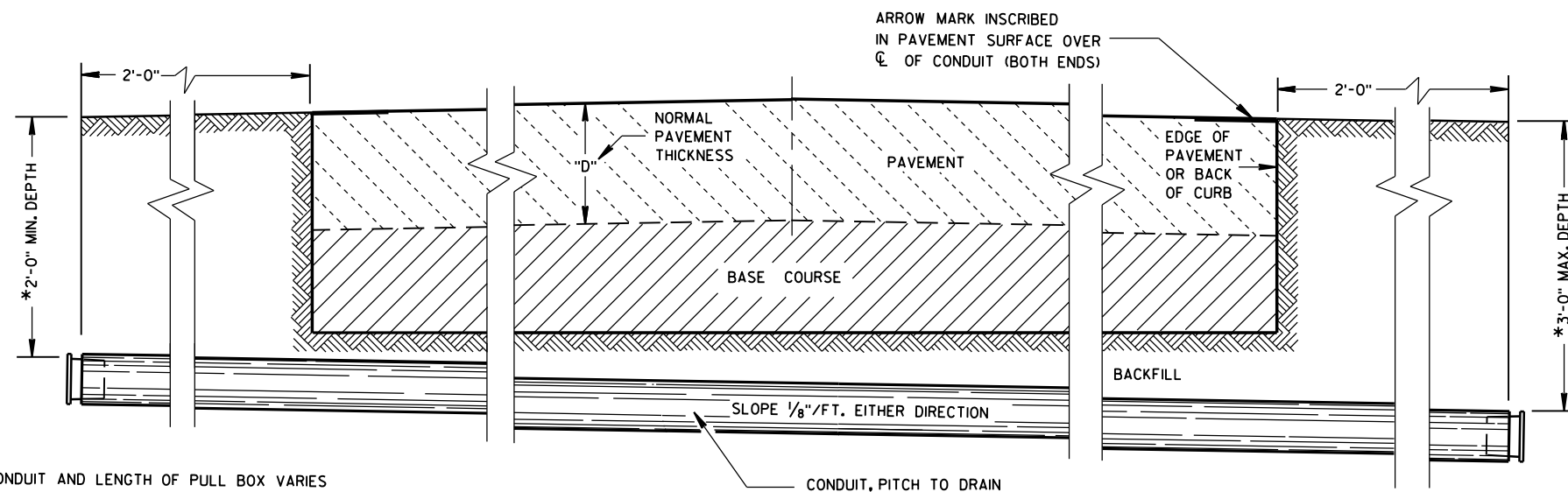
/S/ Beth Cannestra

CHIEF ROADWAY DEVELOPMENT ENGINEER





PLAN VIEW
ARROW MARK



SIDE ELEVATION
DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES
WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSION TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

CONDUIT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March, 2017 /S/ Ahmet Demirbilek
DATE STATE ELECTRICAL ENGINEER
FHWA

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

DIMENSION IN INCHES		CORRUGATED STEEL PIPE								
PIPE DIAMETER (INSIDE)	A	12	12	12	18	18	18	24	24	24
PIPE LENGTH **	B	24	30	36	24	30	36	36	42	48
WALL THICKNESS	C	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 1/4	10 1/4	10 1/4	16 1/4	16 1/4	16 1/4	22 1/4	22 1/4	22 1/4
FRAME	E	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2	26 1/2	26 1/2	26 1/2
FRAME	F	8 1/2	8 1/2	8 1/2	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2
FRAME	G	11 1/2	11 1/2	11 1/2	17 1/2	17 1/2	17 1/2	23 1/2	23 1/2	23 1/2
WEIGHT IN POUNDS *										
FRAME AND COVER		60	60	60	110	110	110	155	155	155

* THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.

** NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR TRAFFIC LOADS.

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

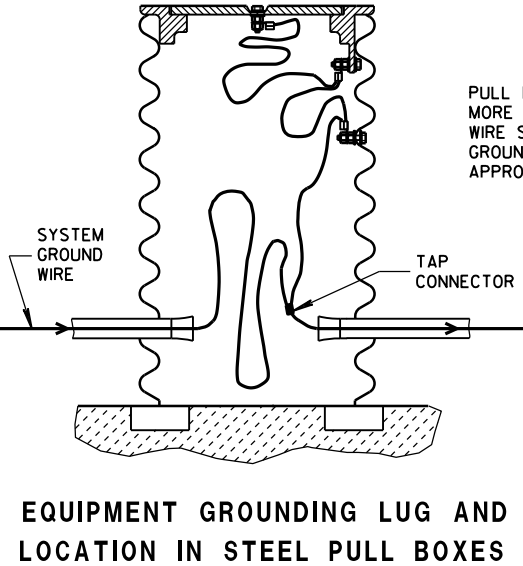
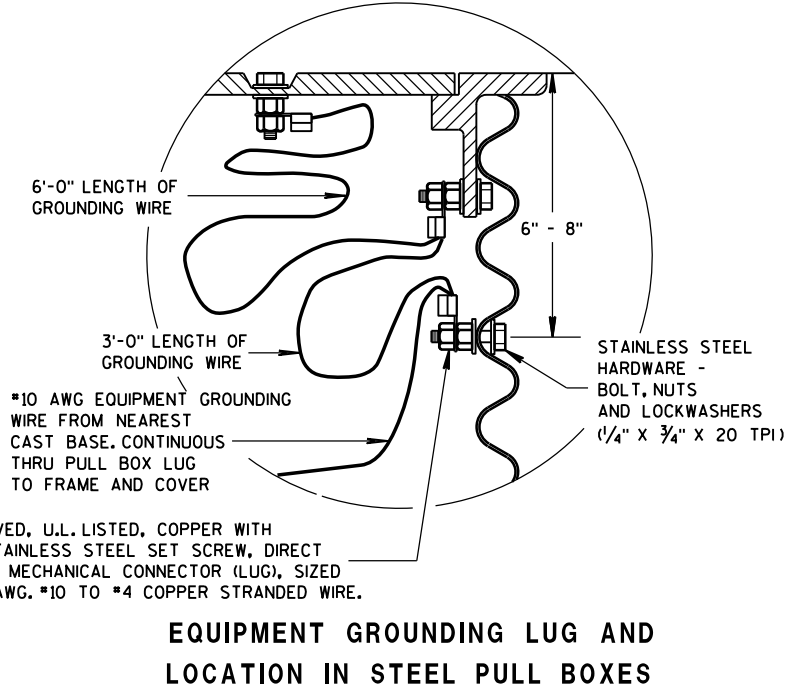
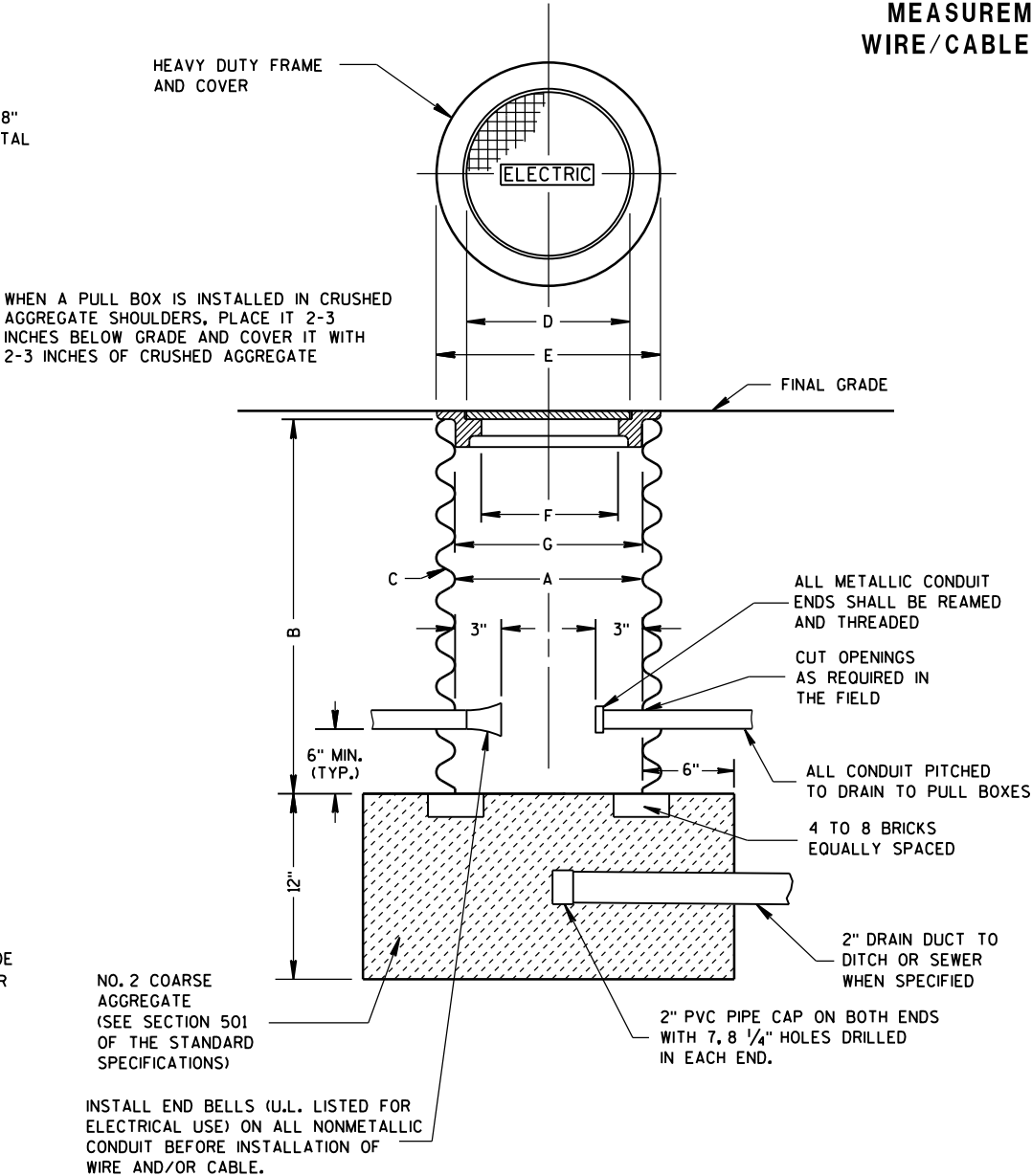
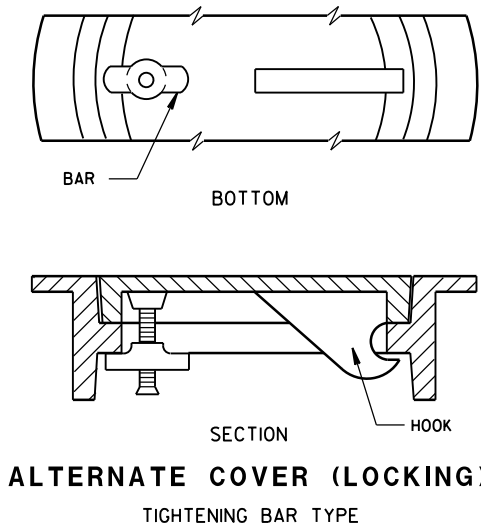
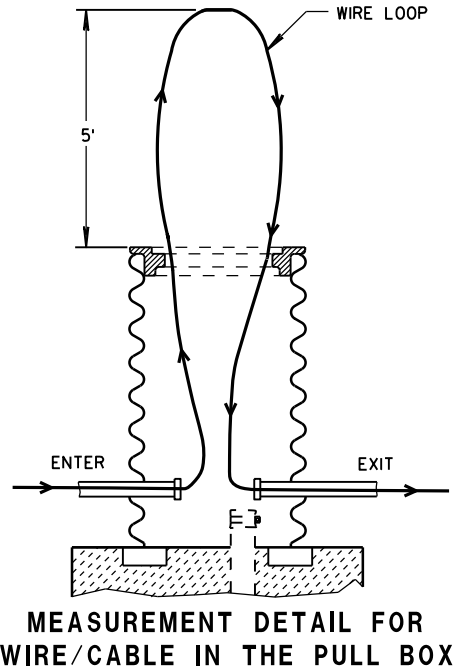
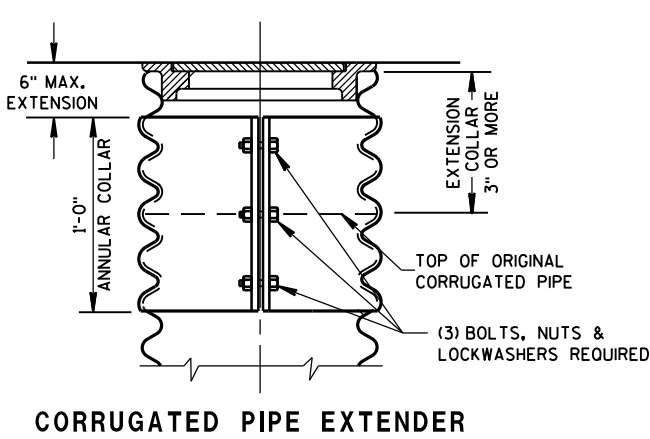
ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

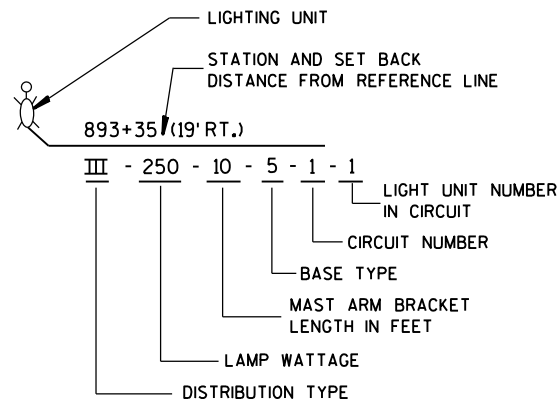
GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED FOR USE WITH COPPER WIRE.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

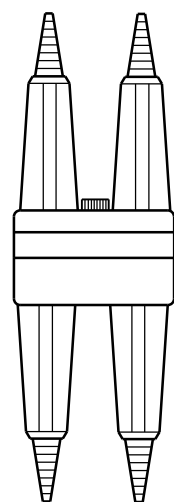
WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.



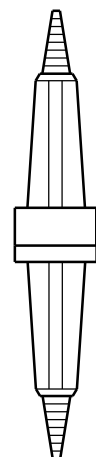
PULL BOX	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED Sept. 2014 DATE	/S/ Ahmet Demirelek STATE ELECTRICAL ENGINEER
FHWA	



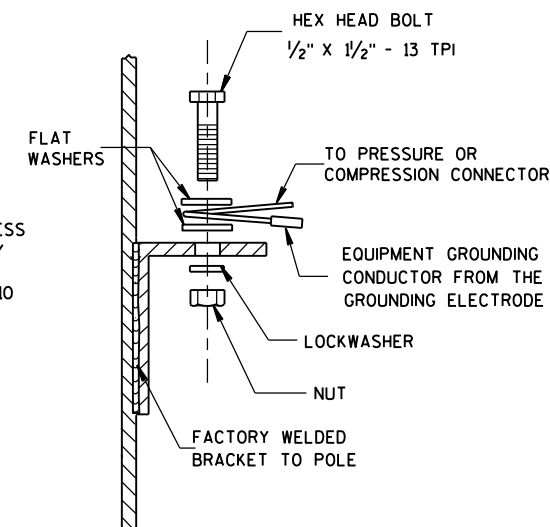
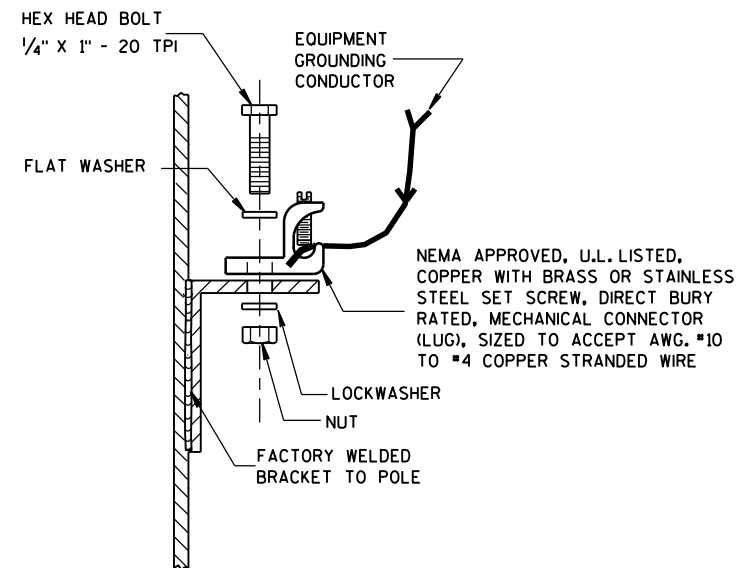
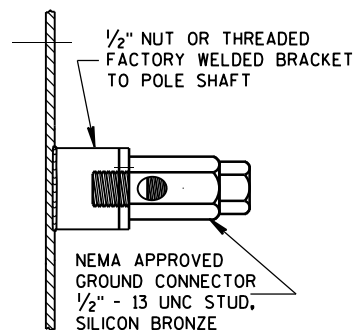
**LIGHTING UNIT CODE
(TYPICAL)**



**DETAIL "A"
BREAKAWY
DOUBLE POLE WITH
WATERPROOF
INSULATING BOOT**



**DETAIL "B"
BREAKAWY
SINGLE POLE WITH
WATERPROOF
INSULATING BOOT**



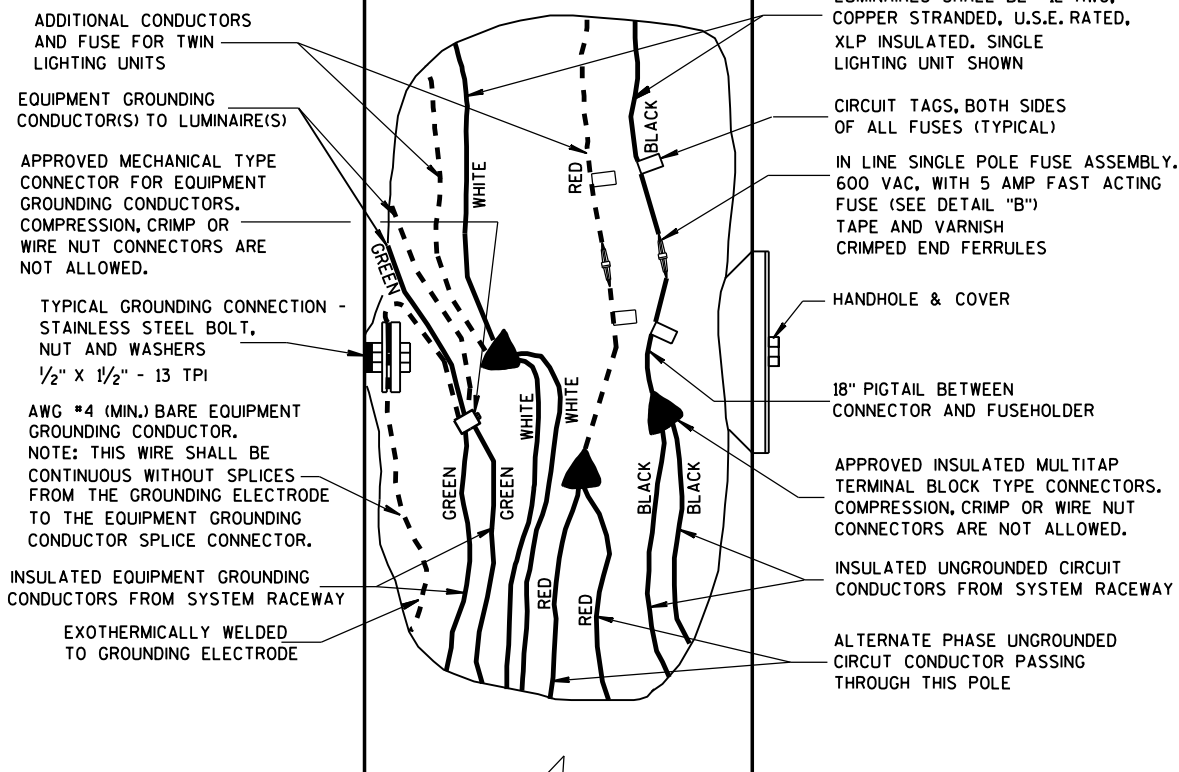
TYPICAL GROUNDING CONNECTIONS
NUT, BOLT, WASHERS AND LOCKWASHERS SHALL BE STAINLESS STEEL

GENERAL NOTES

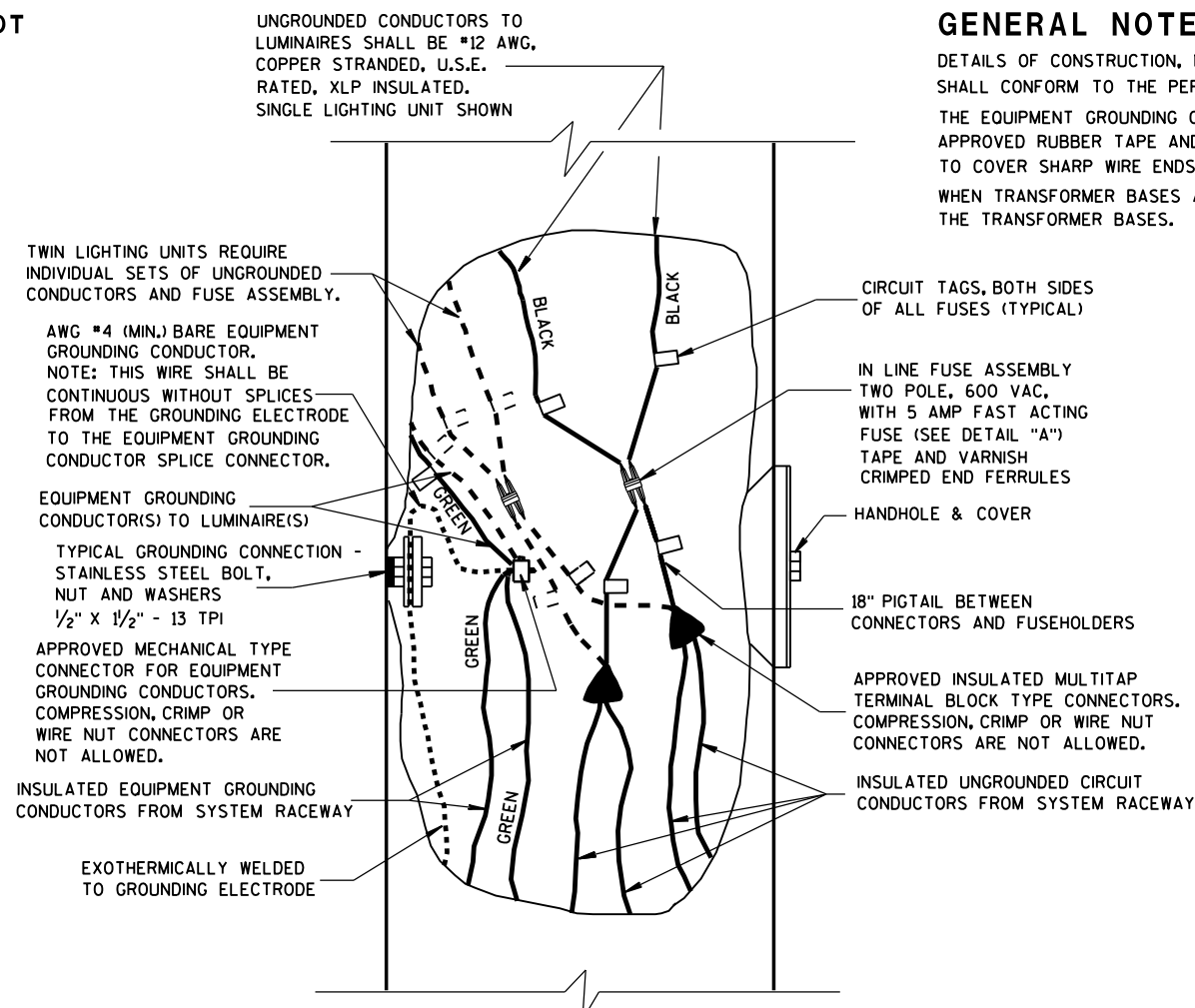
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

THE EQUIPMENT GROUNDING CONNECTOR SHALL BE TAPED WITH 3 WRAPS (MINIMUM) OF APPROVED RUBBER TAPE AND THEN 3 WRAPS (MINIMUM) OF APPROVED VINYL TAPE TO COVER SHARP WIRE ENDS AFTER THE CONNECTION IS COMPLETED.

WHEN TRANSFORMER BASES ARE USED, ALL WIRING CONNECTIONS SHALL OCCUR WITHIN THE TRANSFORMER BASES.



**3 WIRE - 120, 240 OR 480 VAC (UNGROUND CONDUCTOR)
WITH GROUNDED CONDUCTOR AND
WITH EQUIPMENT GROUNDING CONDUCTOR**

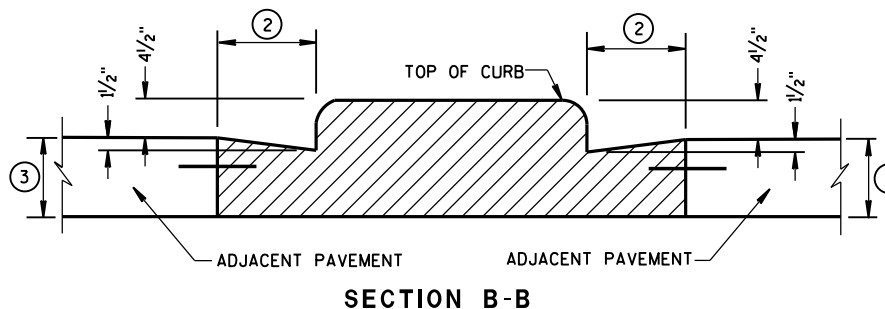
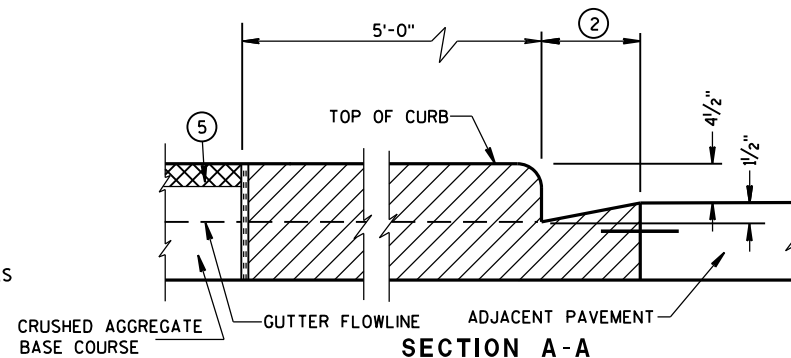
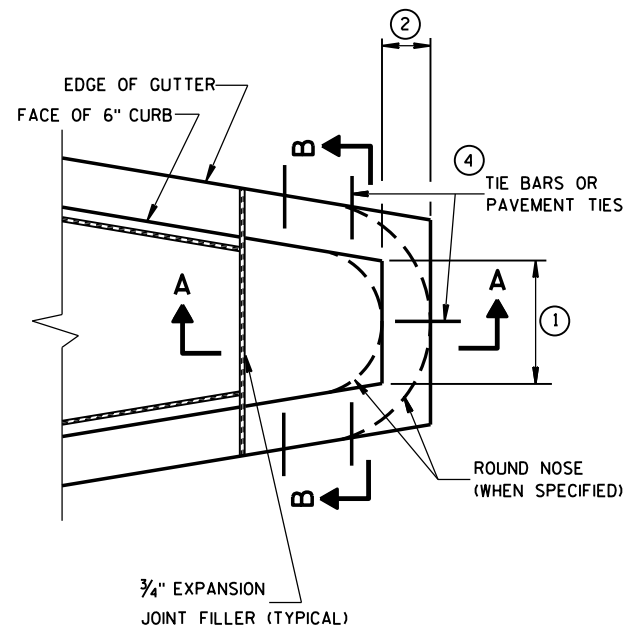
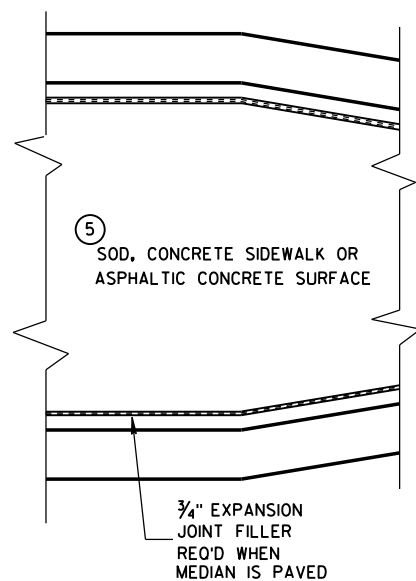


**2 WIRE - 240 OR 480 VAC (UNGROUND CONDUCTORS)
WITH EQUIPMENT GROUNDING CONDUCTOR**

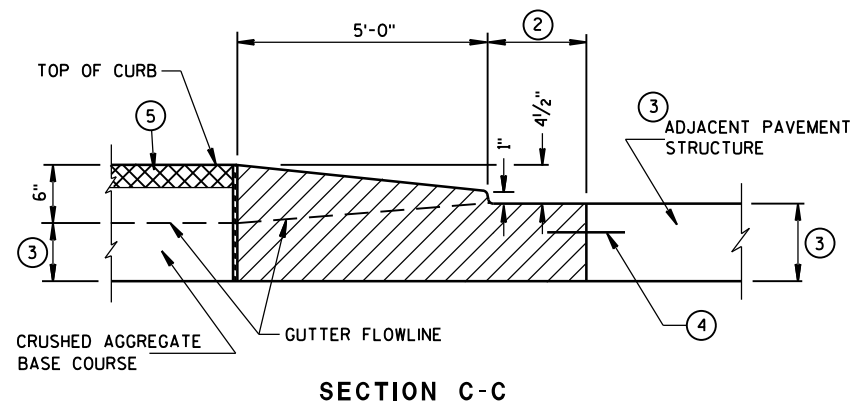
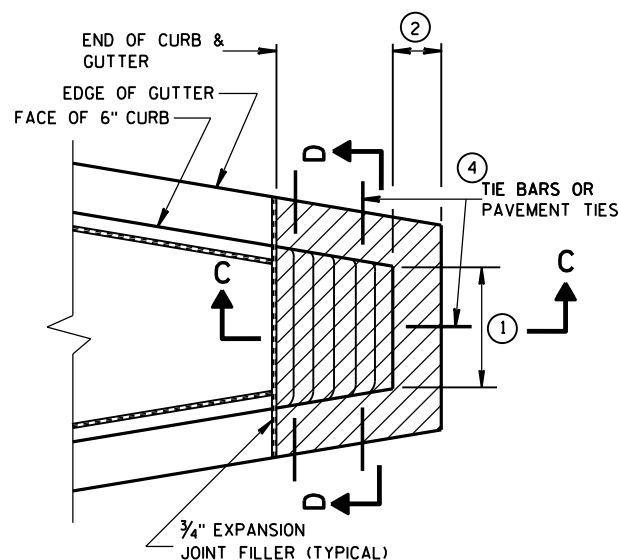
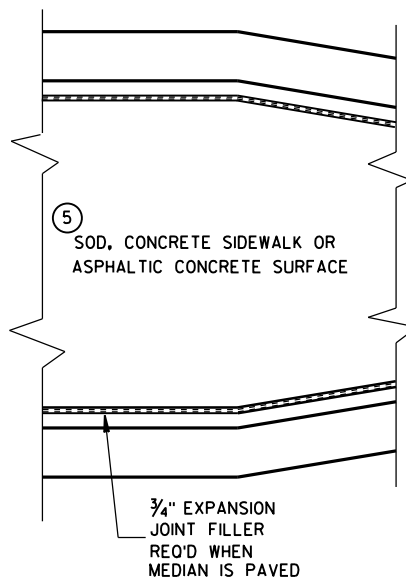
**NON-FREEWAY LIGHTING UNIT
POLE WIRING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

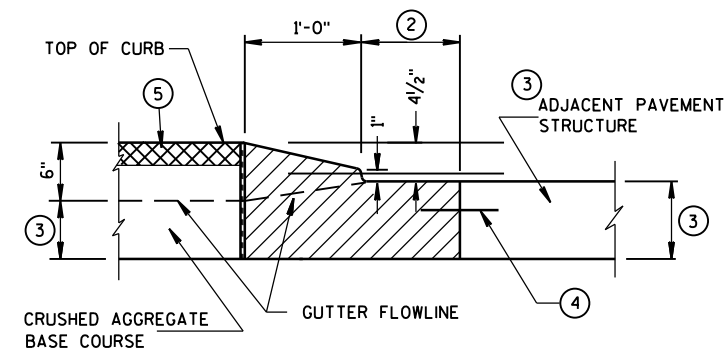
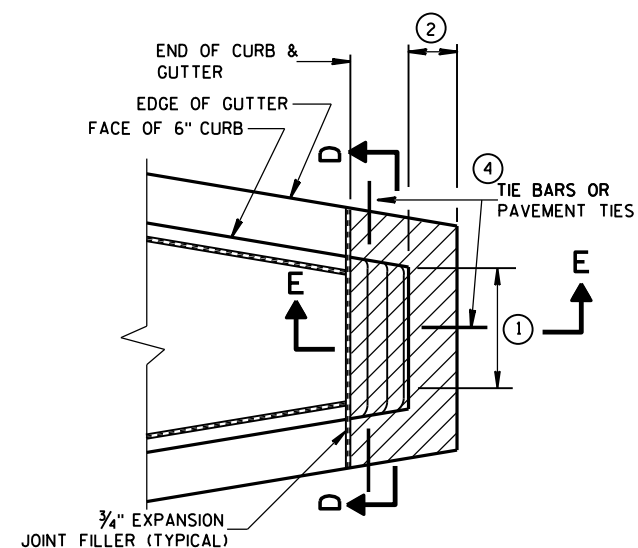
APPROVED
Sept. 2014 /S/ Ahmet Demirbilek
DATE STATE ELECTRICAL ENGINEER
FHWA



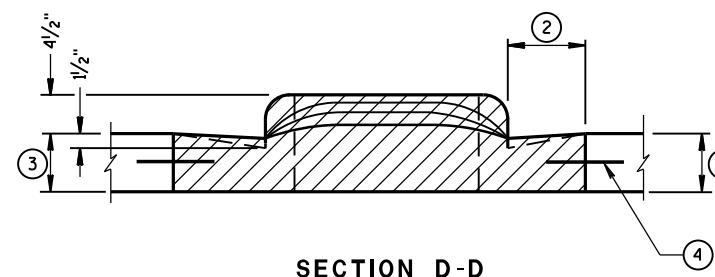
CONCRETE MEDIAN BLUNT NOSE DETAIL



CONCRETE MEDIAN SLOPED NOSE TYPE 1



CONCRETE MEDIAN SLOPED NOSE TYPE 2



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.

- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.

PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.

- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.

CONCRETE MEDIAN NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

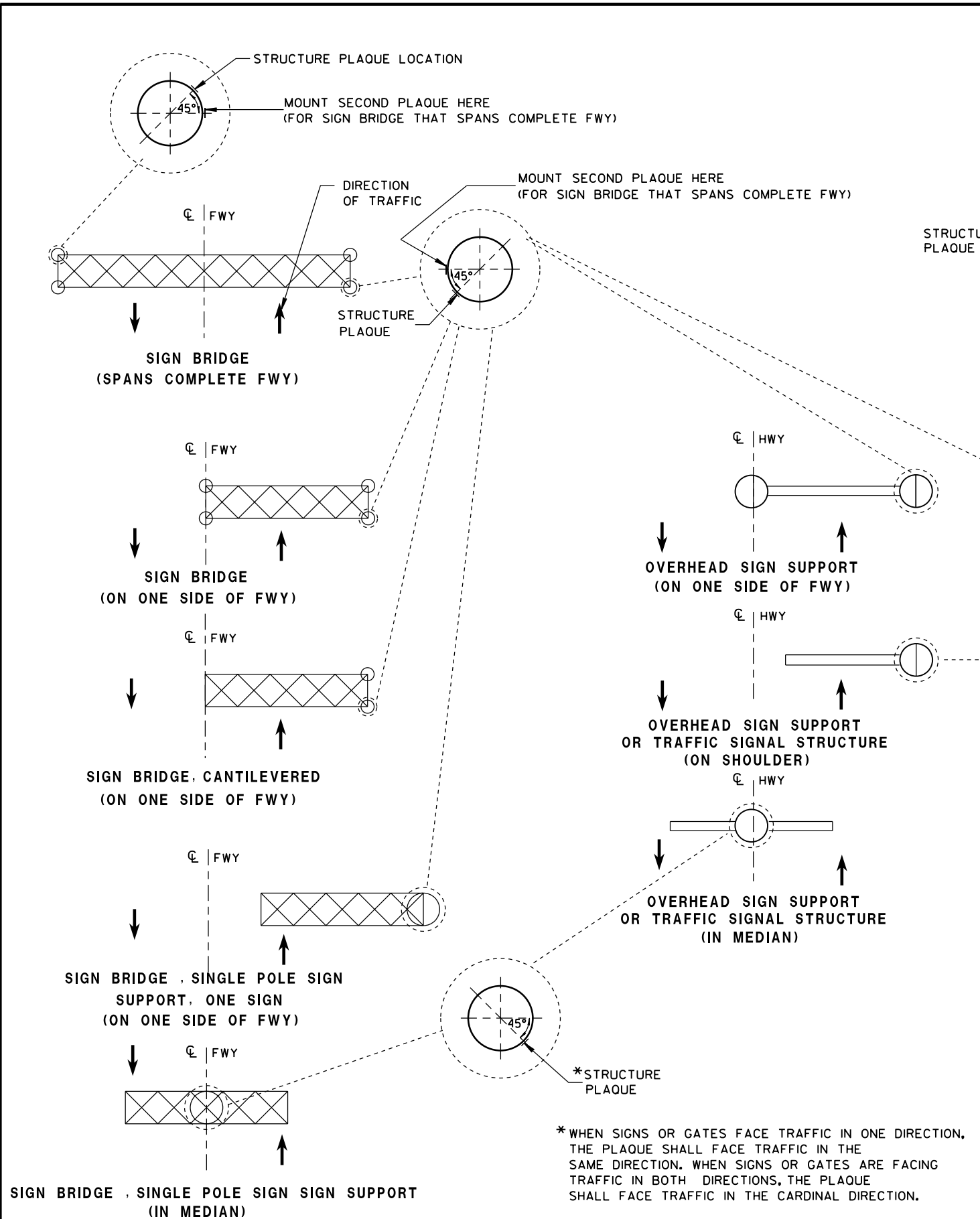
APPROVED

6/8/2006

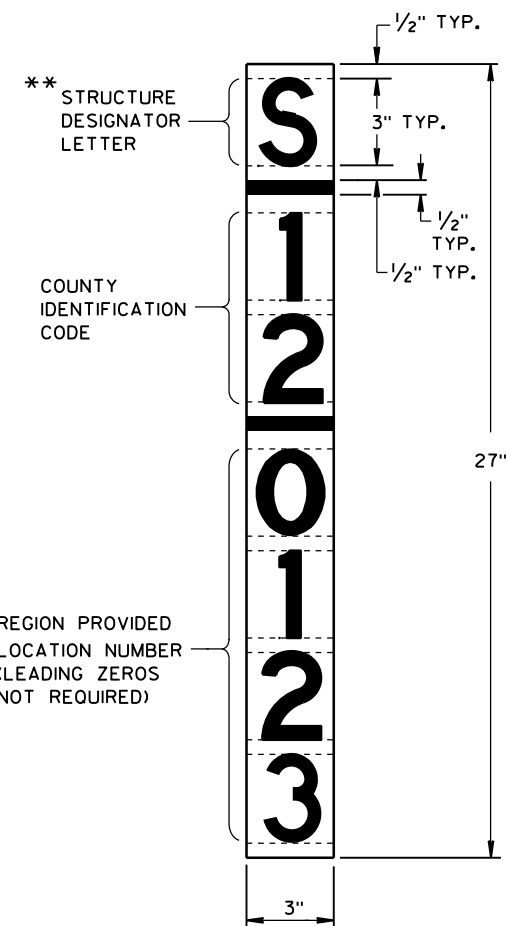
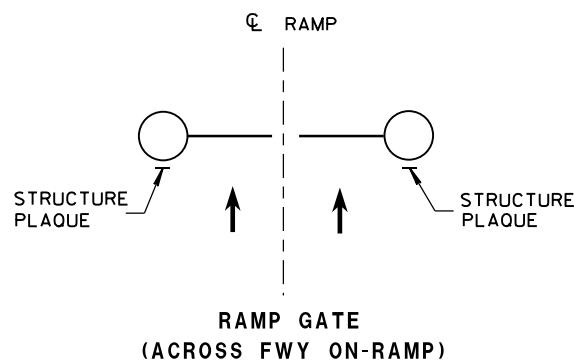
DATE

FHWA

/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



LOCATION OF RAMP GATE, SIGN BRIDGE, OVERHEAD
SIGN SUPPORT & TRAFFIC SIGNAL STRUCTURE PLAQUES



RAMP GATE, SIGN BRIDGE, OVERHEAD SIGN SUPPORT AND TRAFFIC SIGNAL
STRUCTURE PLAQUE FOR SIGN BRIDGES AND OVERHEAD SIGN
SUPPORT WHICH ARE NOT STRUCTURE MOUNTED

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN IN THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PLAQUES SHALL BE INCIDENTAL TO ALL NEW INSTALLATIONS.

IF THE PROPOSED SIGN BRIDGE OR OVERHEAD SIGN SUPPORT IS REPLACING AN EXISTING SIGN BRIDGE OR OVERHEAD SIGN SUPPORT, A NEW IDENTIFICATION PLAQUE WILL BE REQUIRED.

FASTEN TOP, CENTER AND BOTTOM OF PLAQUE TO POLE OR OTHER LOCATION AS FOLLOWS:

GALVANIZED STEEL SHAFT - 3 STAINLESS STEEL POP RIVETS

A588 STEEL SHAFT - SHIM FOR DRAINAGE WITH STAINLESS WASHERS;
FASTEN WITH STAINLESS SELF-TAPPING SCREWS

ALUMINUM SHAFTS - 3 ALUMINUM POP RIVETS

MOUNTING HEIGHT SHALL BE APPROXIMATELY 5.0' ABOVE CURB OR SHOULDER. ADJUST IF IT IS KNOWN THAT REQUIRED TRAFFIC SIGNS WILL OBSTRUCT.

PLAQUE MATERIALS:

BASE - SHEET ALUMINUM, 0.060" THICK.

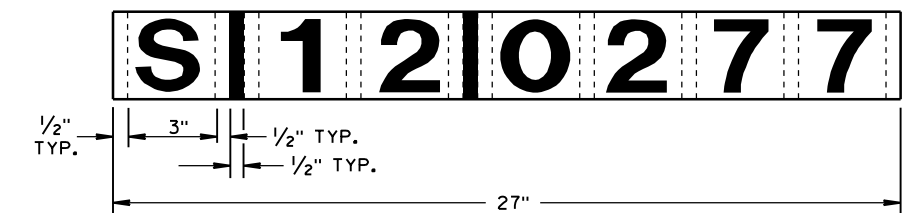
FACE - WHITE, SELF-ADHESIVE VINYL SHEETING, NON-RETROREFLECTIVE

LINES - BLACK, 1/2" WIDE, SELF-ADHESIVE

CHARACTERS:- BLACK, SELF ADHESIVE, SERIES "D", SIZE AS SHOWN.

FOR SIGN BRIDGES, STRUCTURE MOUNTED, THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY AS SHOWN ON THE DRAWING. THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY TO THE BACK OF THE SIGN, BETWEEN THE ALUMINUM EXTRUSIONS, NEAR THE TOP LEFT HAND CORNER OF THE SIGN. THE BASE MATERIAL SHALL BE OMITTED AND THE FACE ADHERED DIRECTLY TO THE ALUMINUM SURFACE. PRIOR TO ADHERING THE MATERIAL, THE ALUMINUM SURFACE SHALL BE SMOOTH, CLEAN AND DRY.

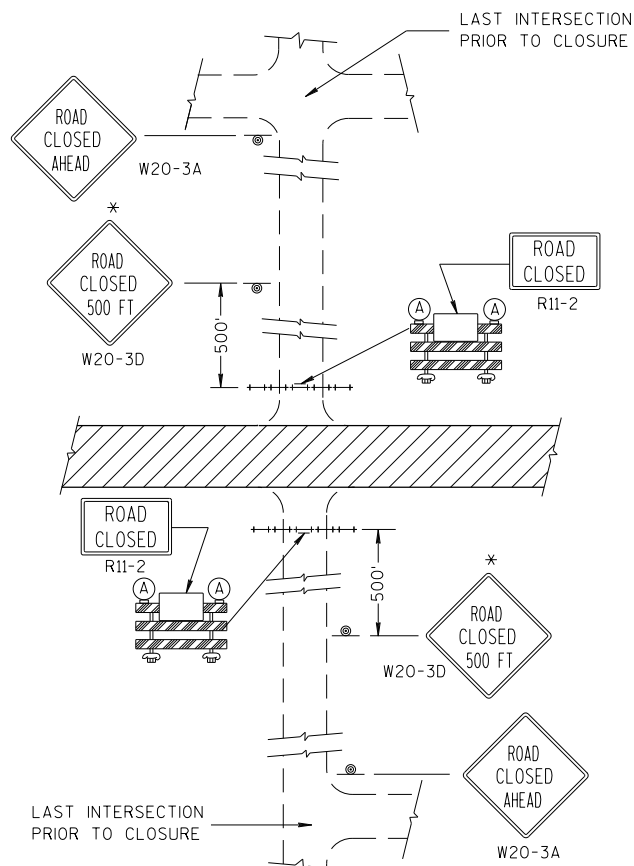
WHERE SIGN BRIDGE ILLUMINATION IS PROVIDED, THE STRUCTURE MUST ALSO HAVE A SIGN BRIDGE CIRCUIT PLAQUE AS SHOWN IN THE ELECTRICAL DETAILS.



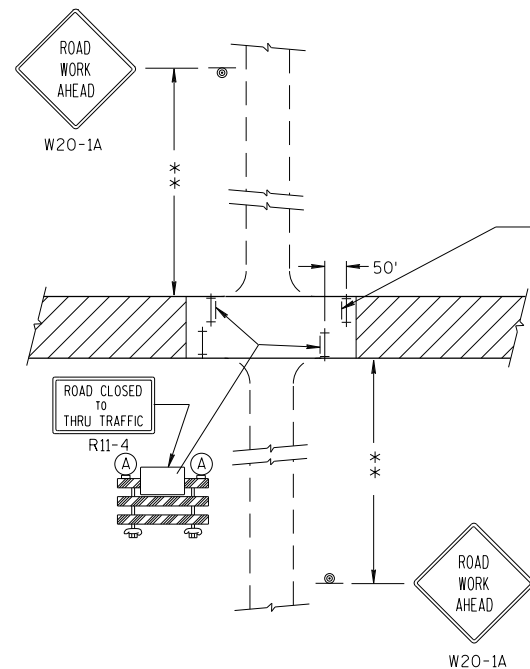
IDENTIFICATION PLAQUE FOR SIGN BRIDGE,
STRUCTURE MOUNTED

** LETTER "G" UTILIZED FOR RAMP GATES.
LETTER "S" UTILIZED FOR SIGN BRIDGES,
OVERHEAD SIGN SUPPORTS, AND TRAFFIC
SIGNALS.

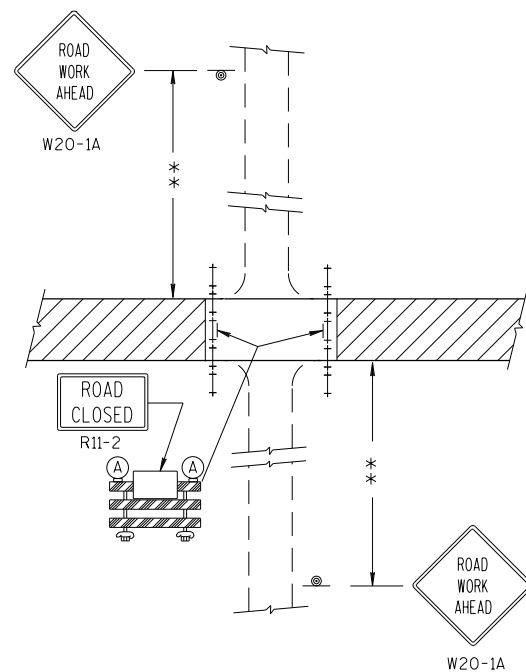
STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES, OVERHEAD SIGN SUPPORTS, & TRAFFIC SIGNALS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 12/4/2012 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



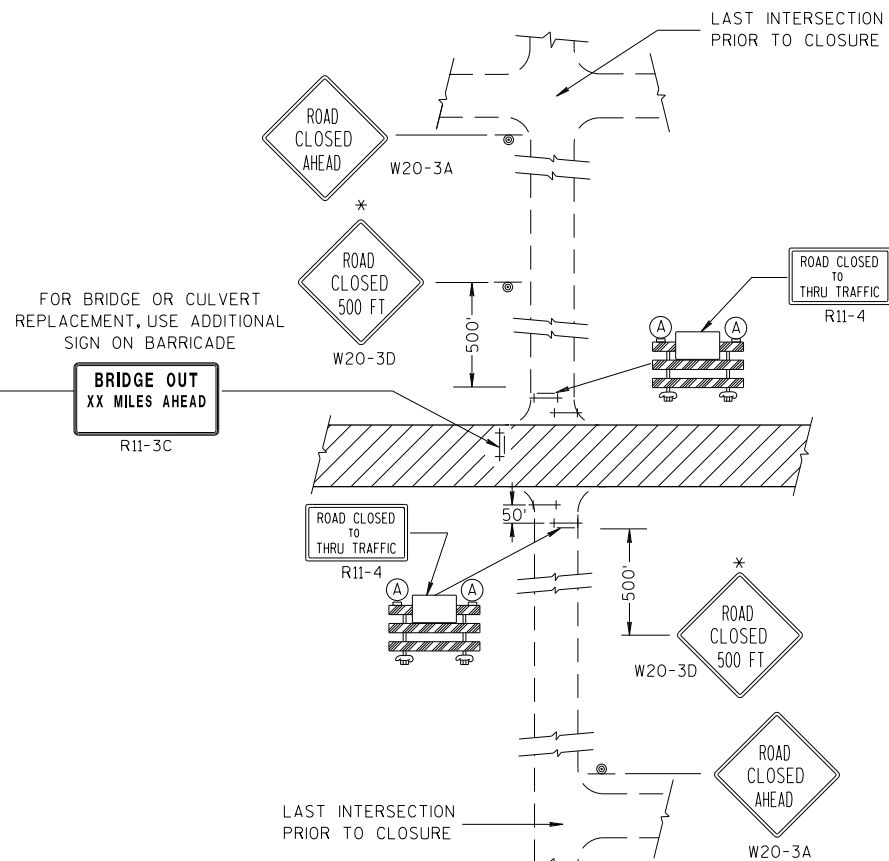
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT).



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

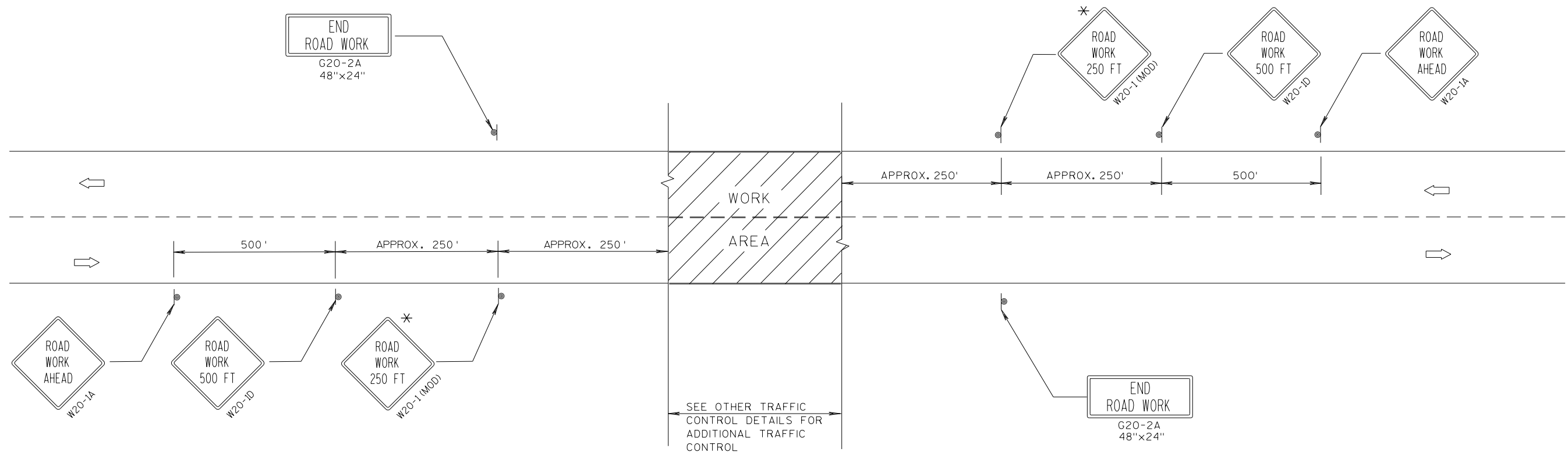
LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER
FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

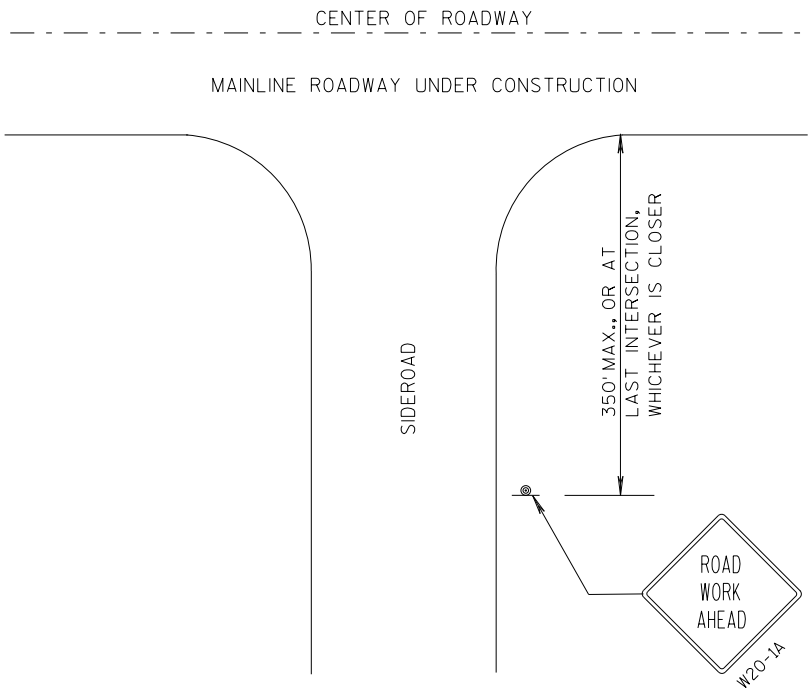
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



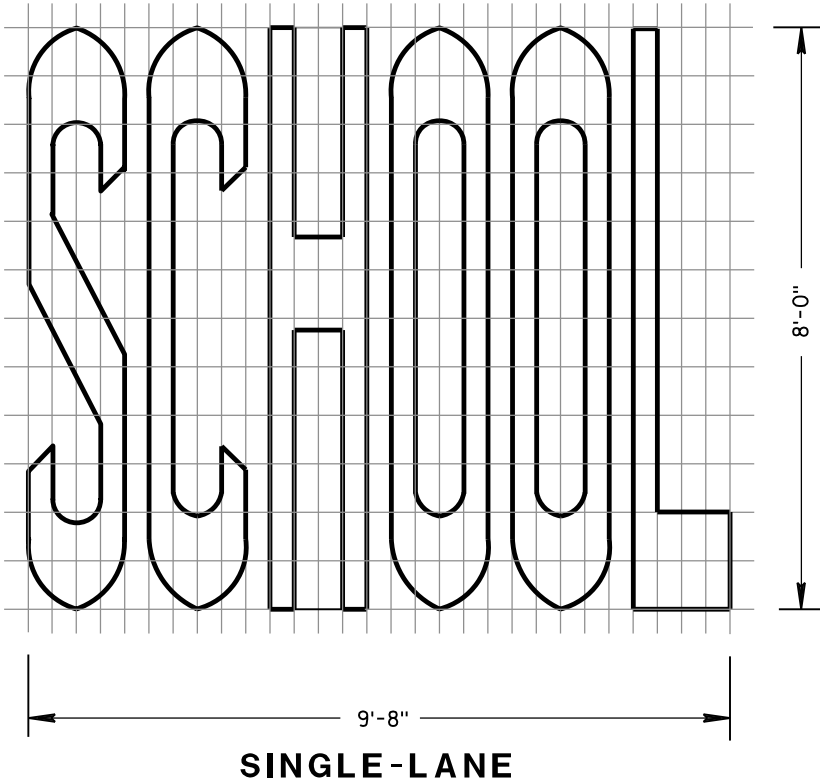
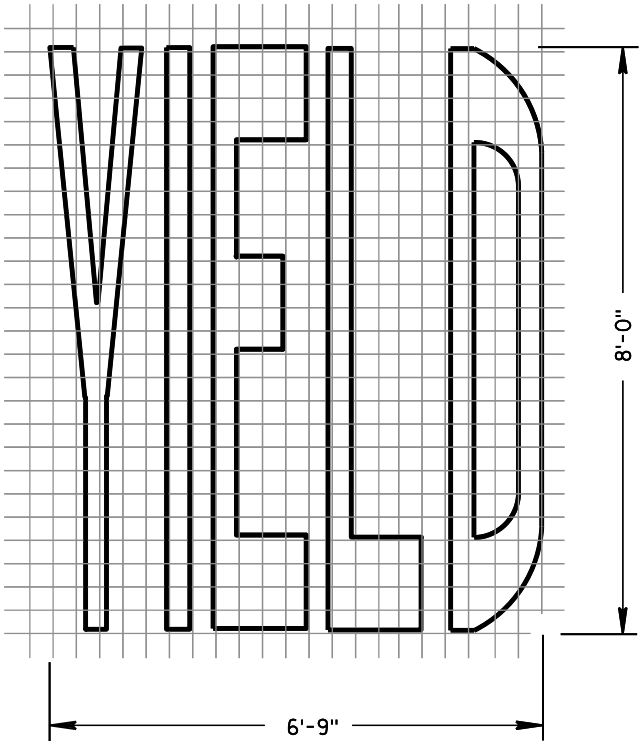
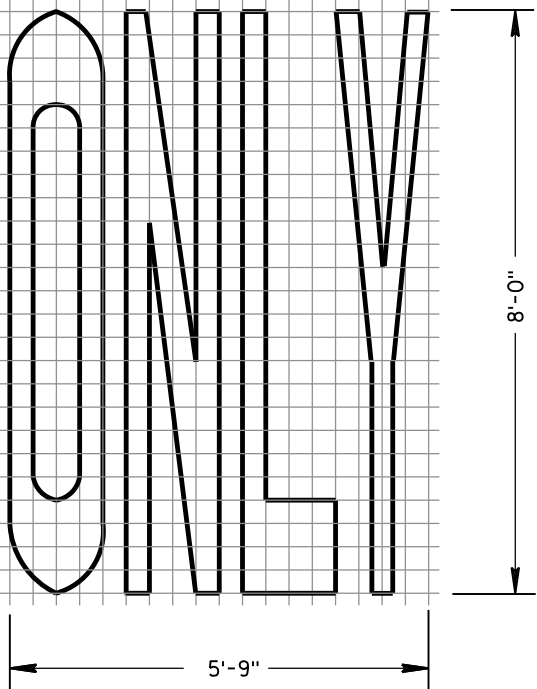
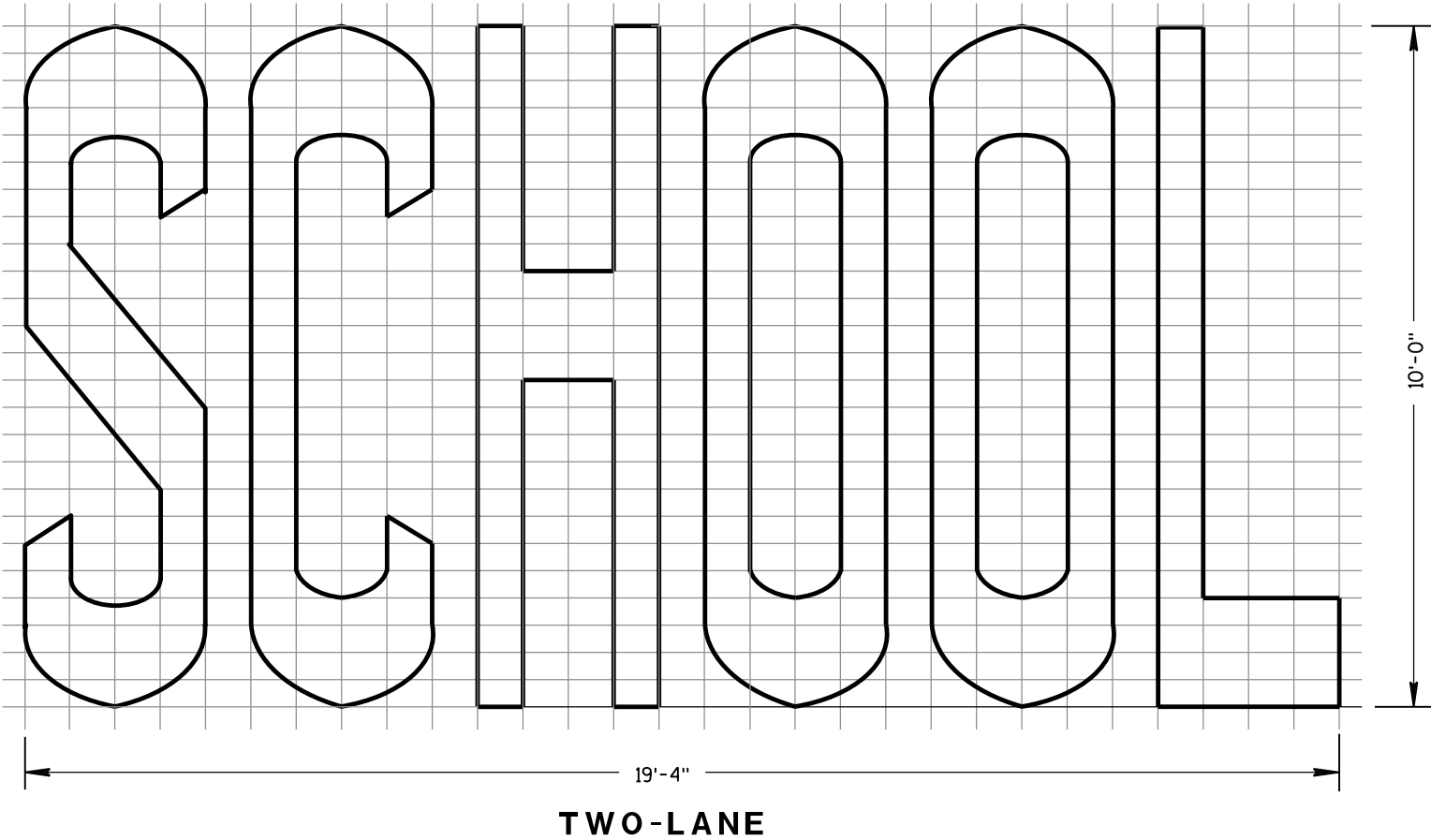
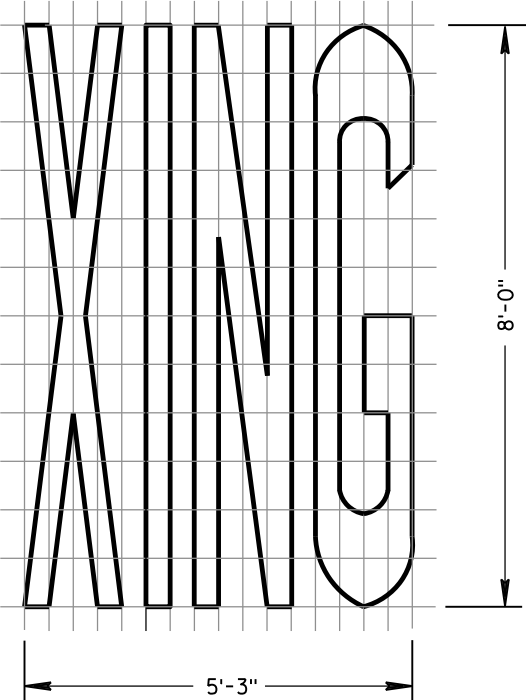
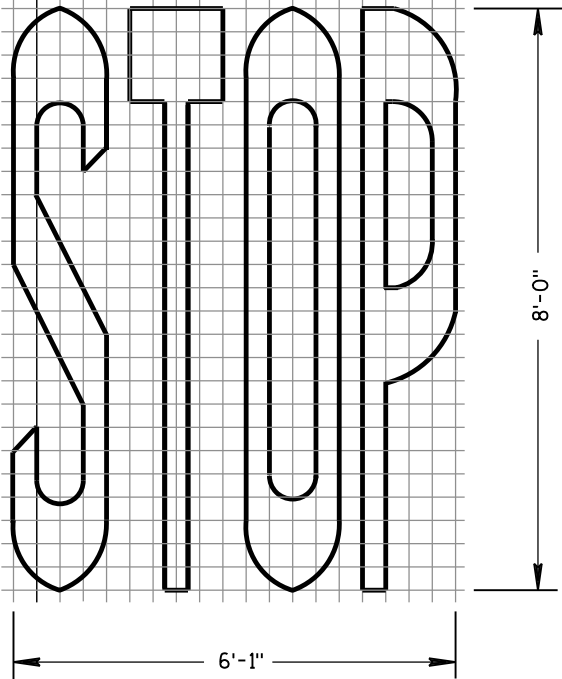
LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

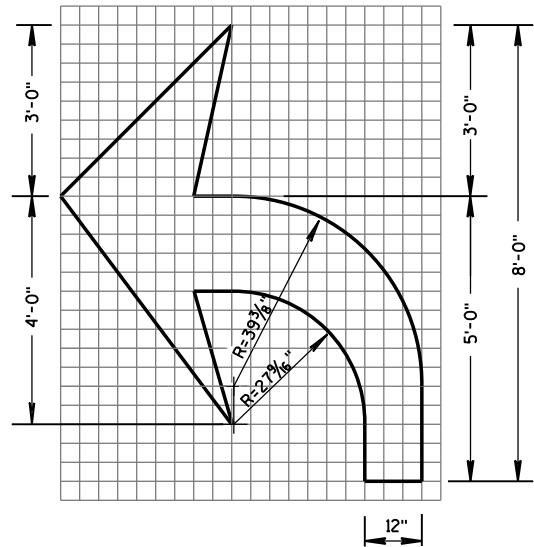
TRAFFIC CONTROL. ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

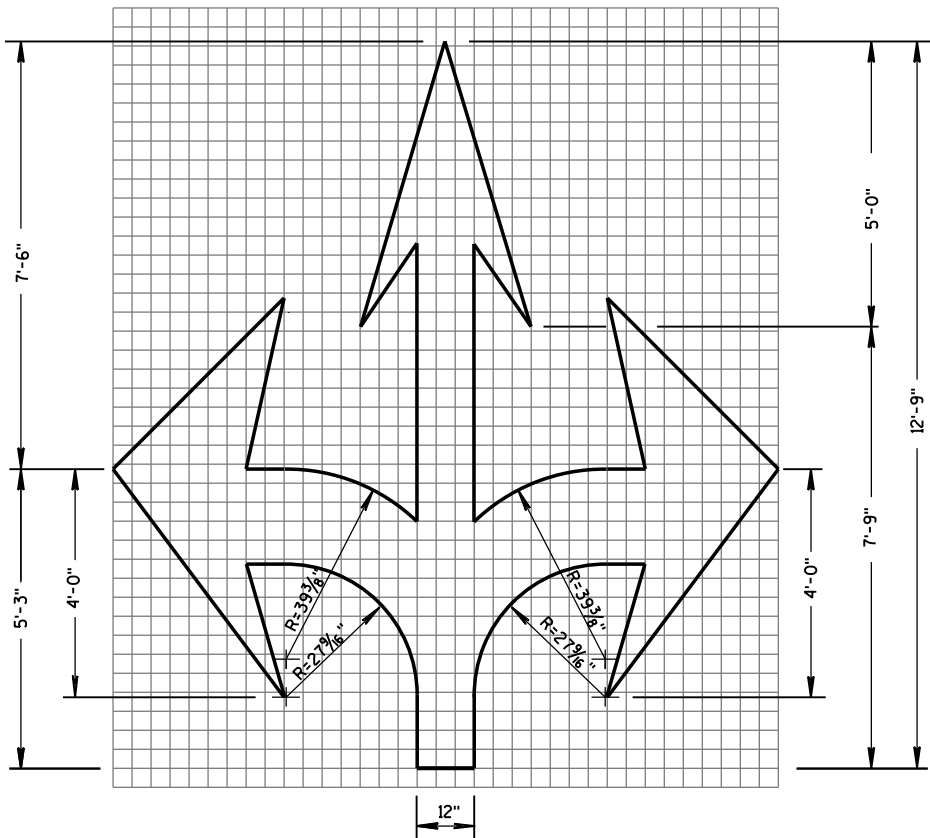
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



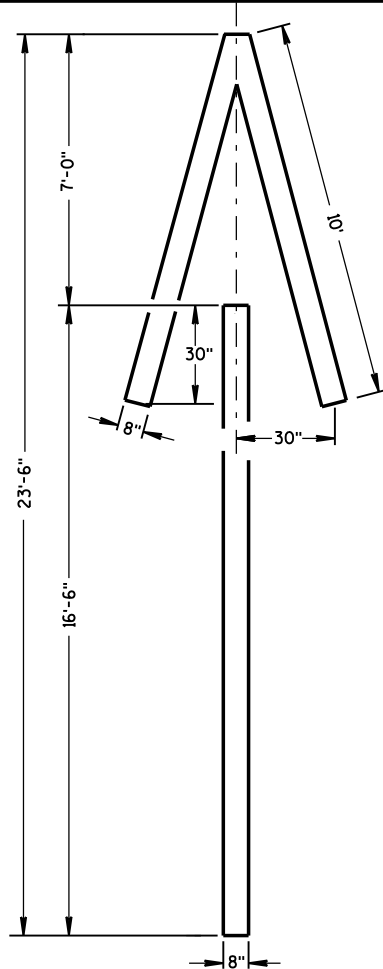
PAVEMENT MARKING WORDS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Matthew R. Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	



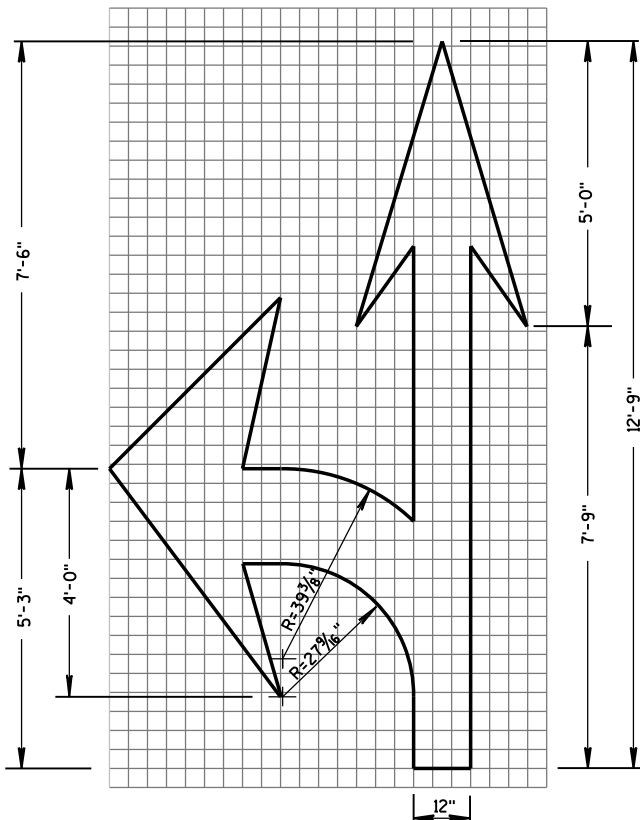
TYPE 2



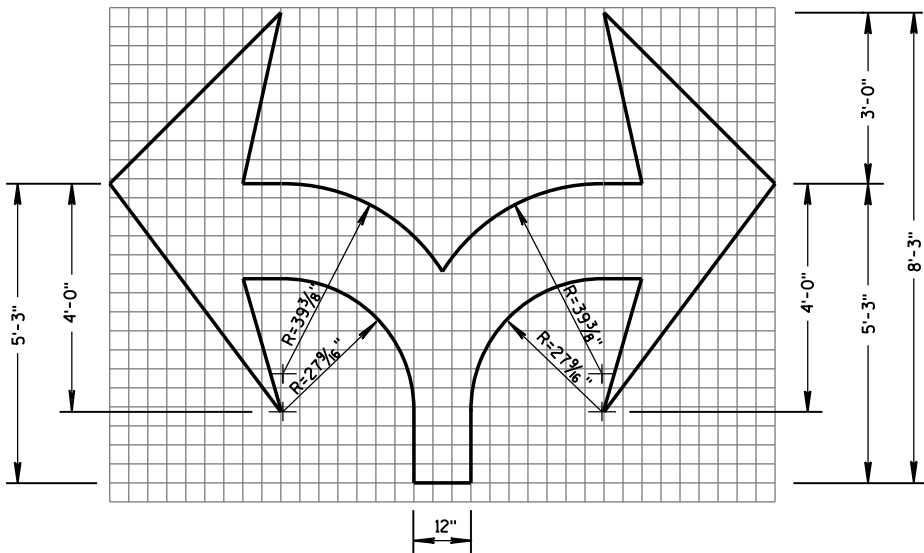
TYPE 6



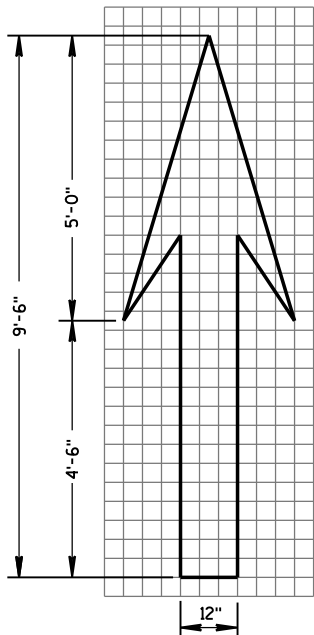
TYPE 4



TYPE 3



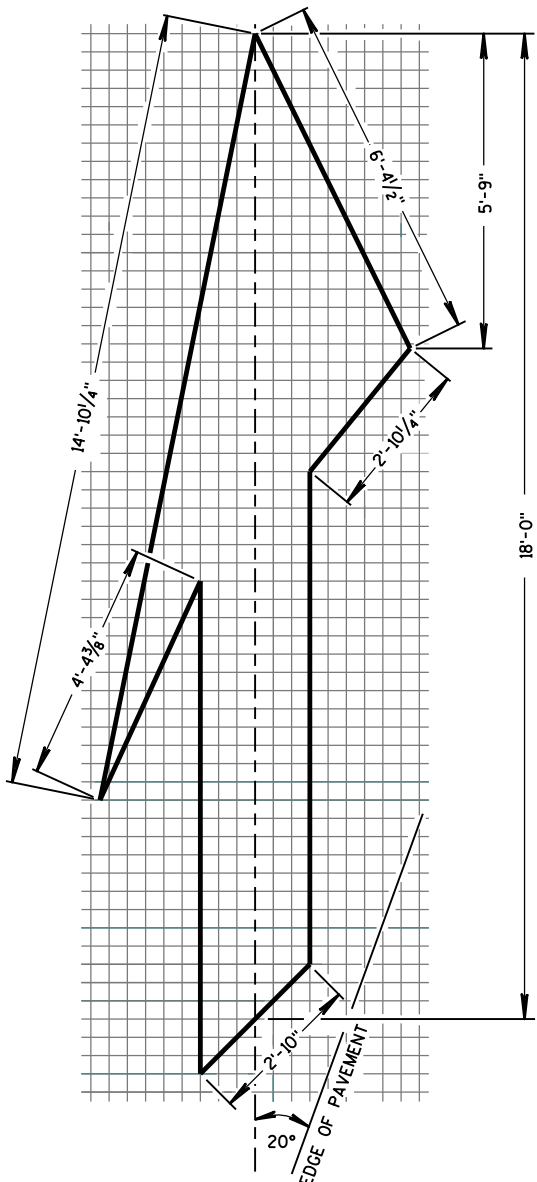
TYPE 7



TYPE 1

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

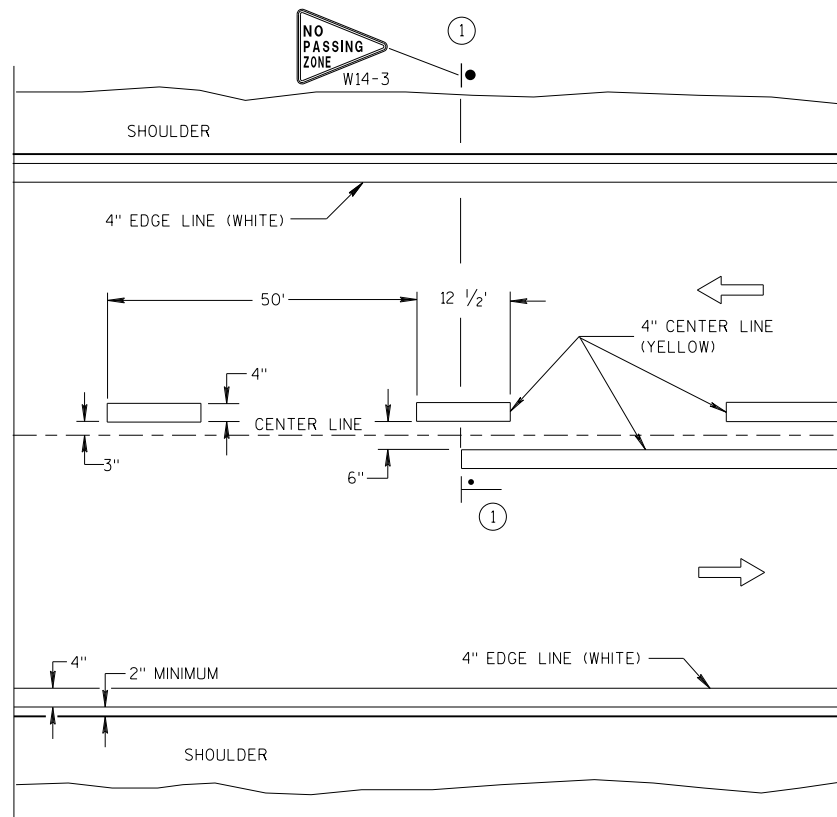


TYPE 5 LANE DROP ARROW

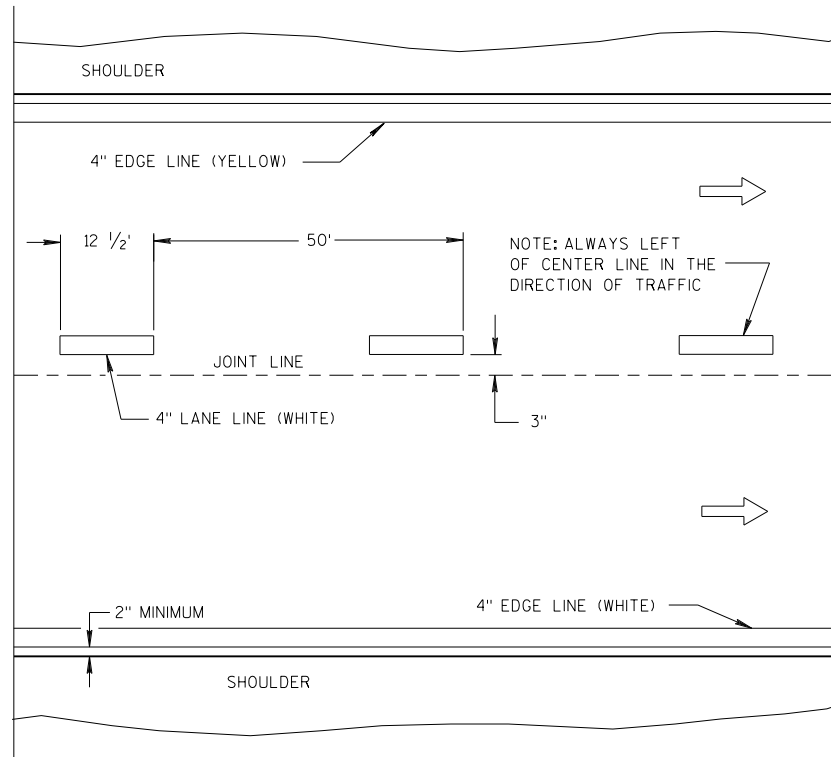
PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

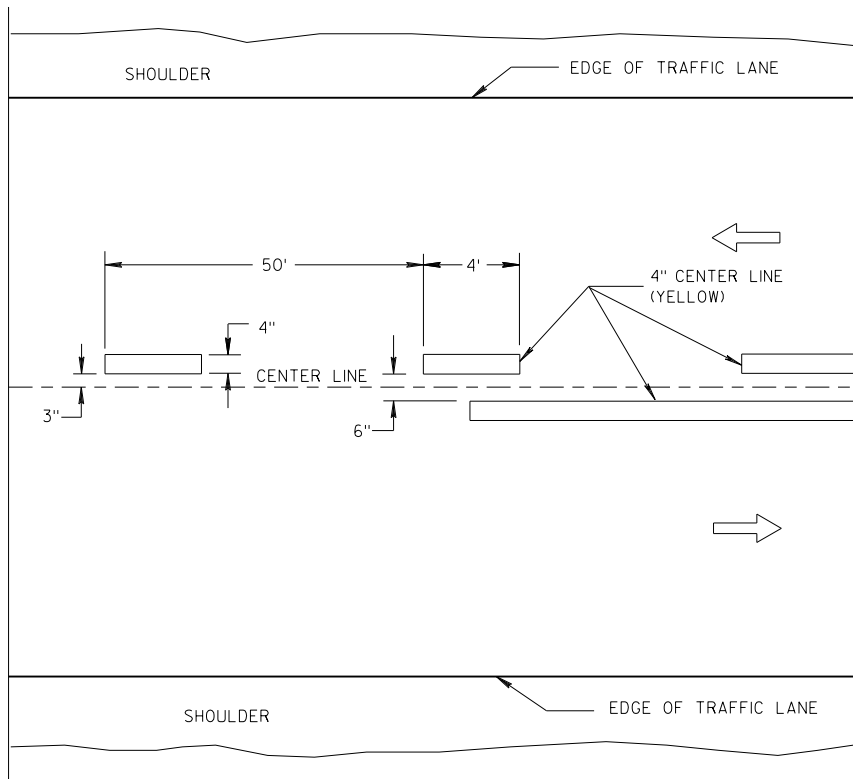


TWO WAY TRAFFIC

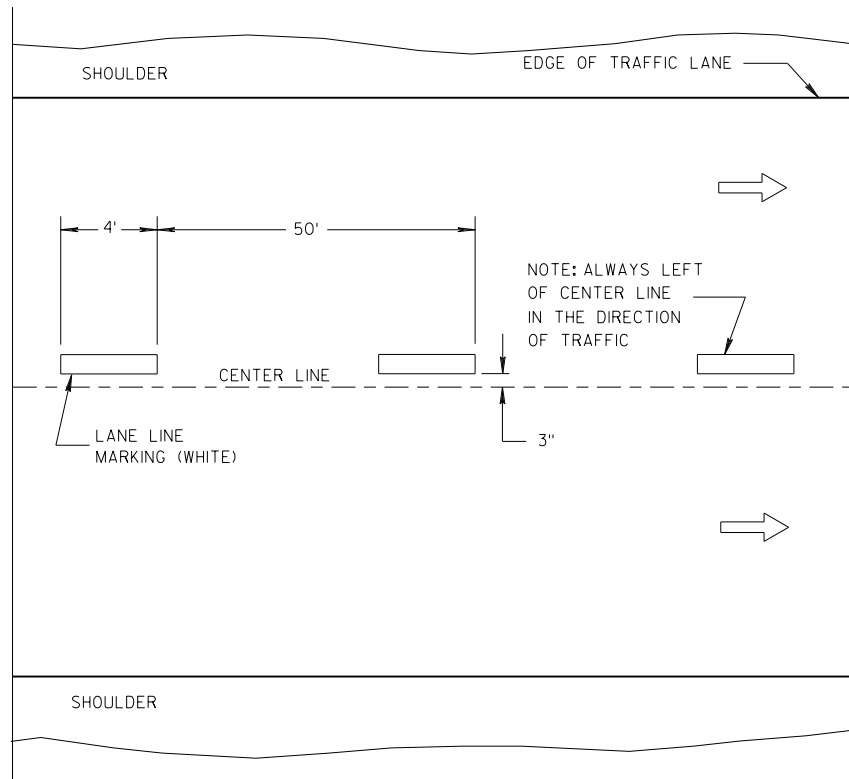


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

LEGEND

- "T" MARKING
- POST MOUNTED SIGN

LONGITUDINAL MARKING
(MAINLINE)

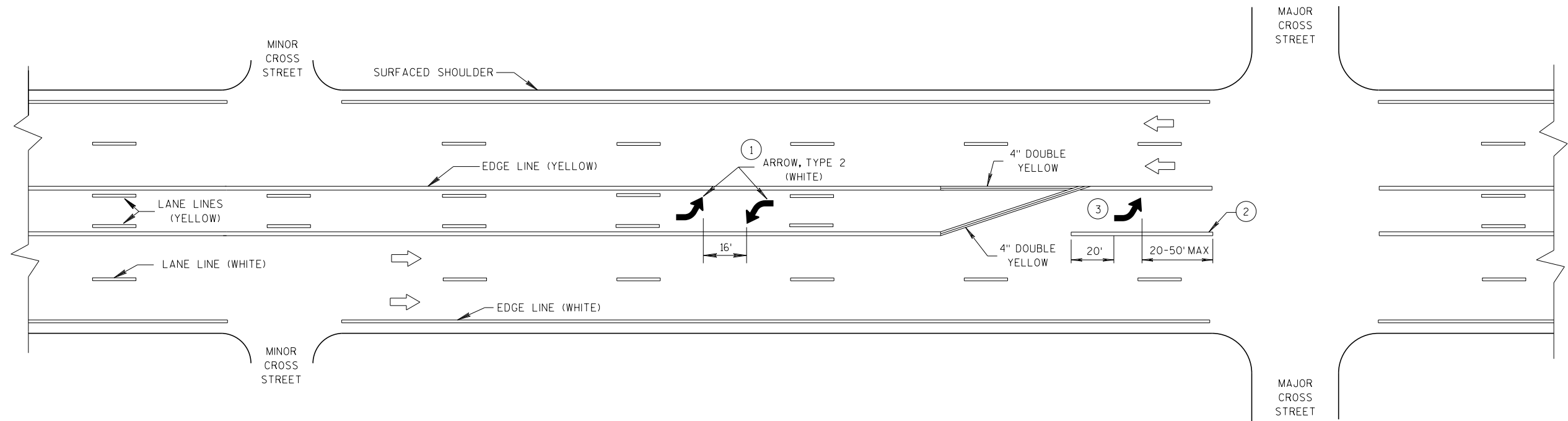
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

GENERAL NOTES

- 1 A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- 2 8" WHITE
- 3 TURN BAY LENGTH OF LESS THAN 48'DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT

DIRECTION OF TRAFFIC



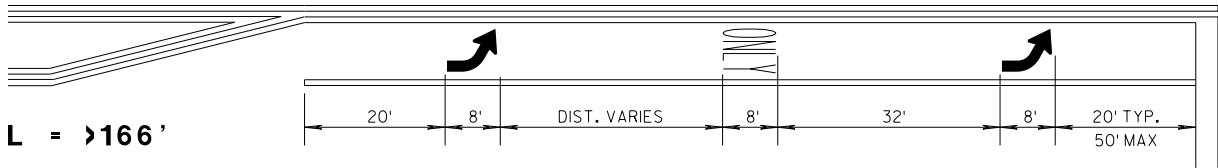
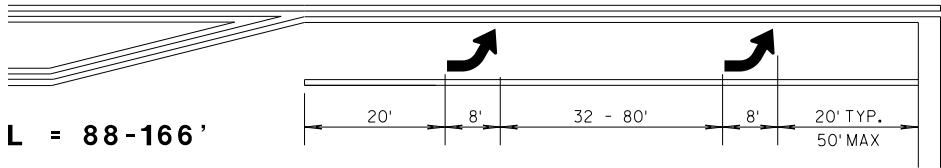
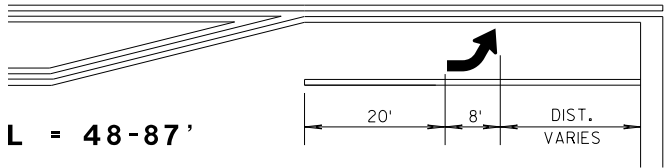
TWO WAY LEFT TURN LANE

PAVEMENT MARKING
(TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

TURN LANE OPTIONS

LENGTH OF TURN BAY (L) OF 0-47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



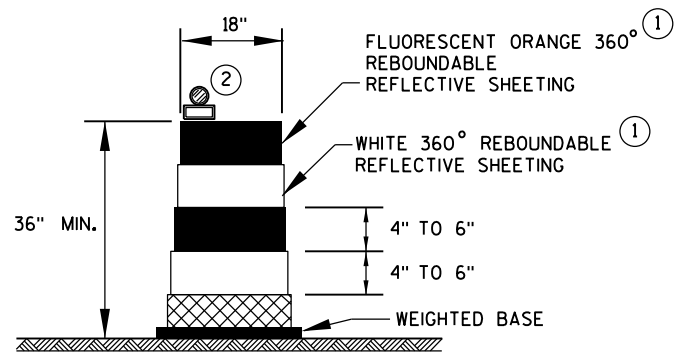
*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

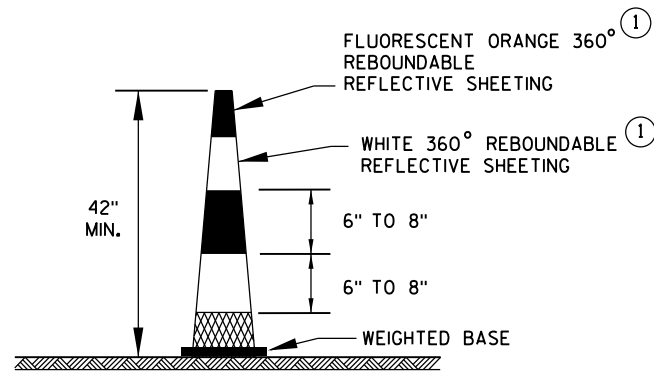
- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROW ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION, THE ARROWS AND ONLY MARKING ARE ELIMINATED.

→ DIRECTION OF TRAFFIC
L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



DRUM

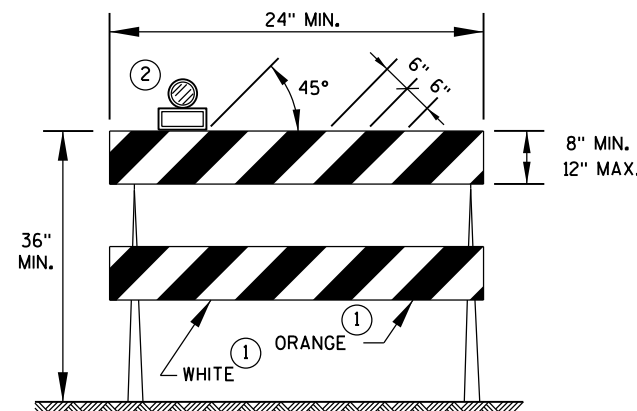


42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS

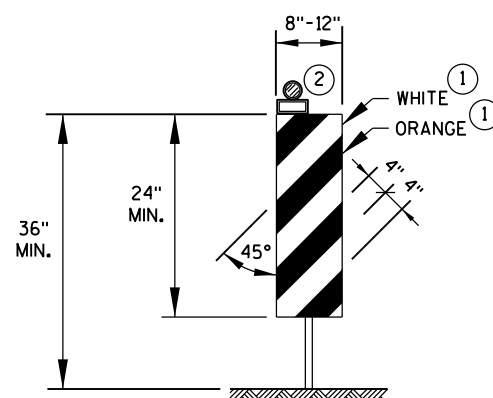
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



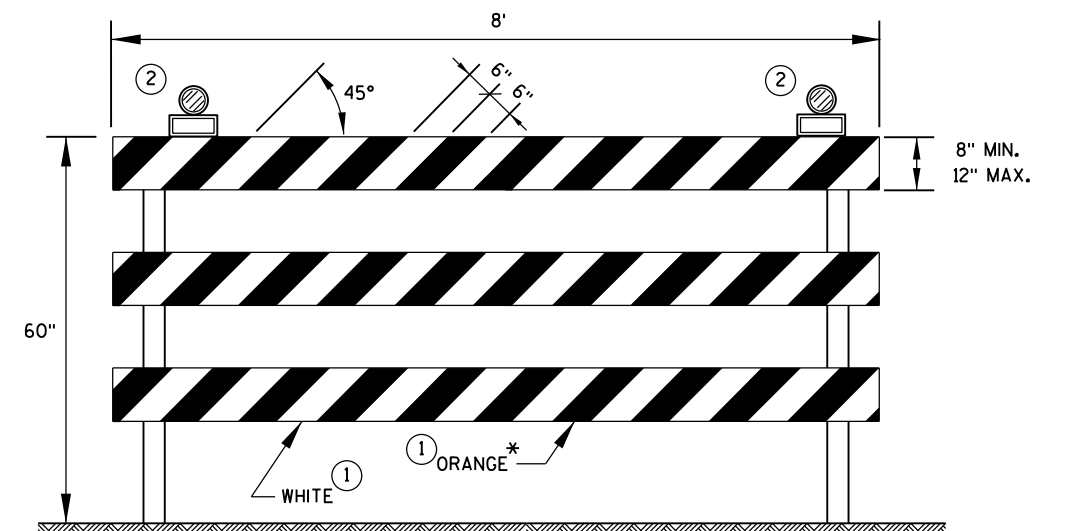
TYPE 2 BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE 3 BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017
DATE

FHWA

/S/ Andrew Heidtke
WORK ZONE ENGINEER

LEGEND

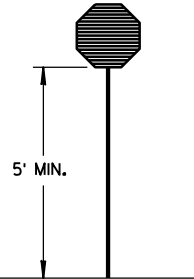
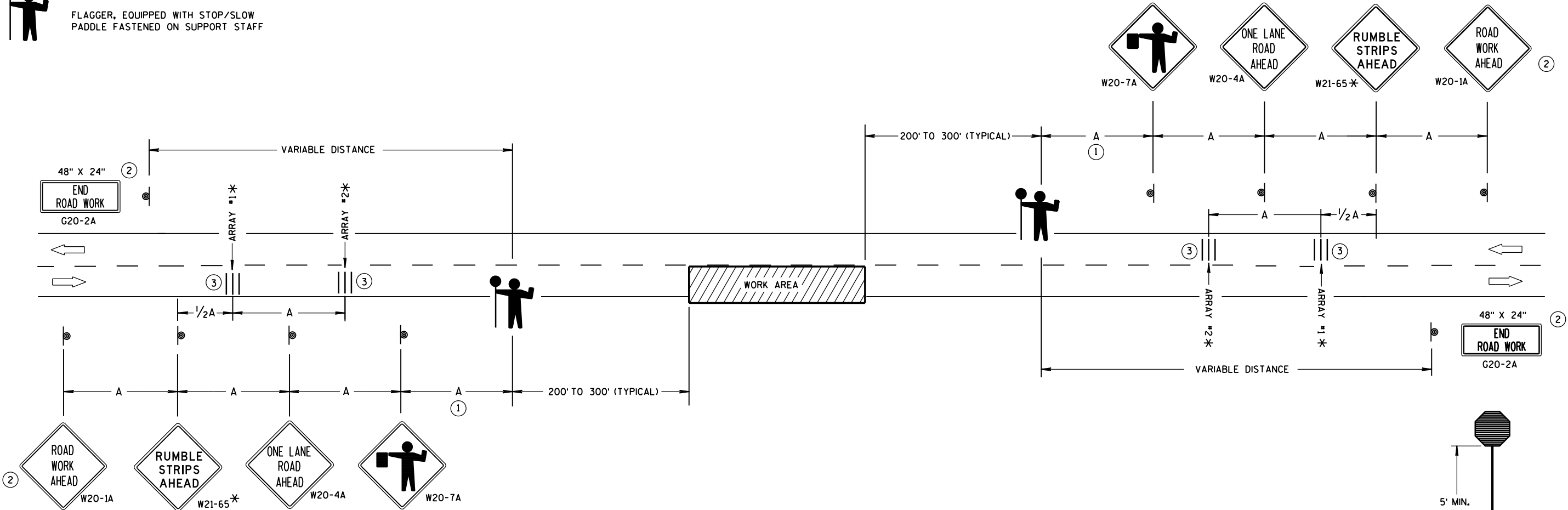
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING A
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING A.



STOP/SLOW PADDLE ON SUPPORT STAFF

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"W0" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS. PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* UTILIZE TEMPORARY RUMBLE STRIPS WHEN FLAGGING OPERATION IS ANTICIPATED TO BE STATIONARY IN EXCESS OF TWO HOURS.

- ① FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS (IF USED) SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ③ EACH TEMPORARY RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heldtke
DATE WORK ZONE ENGINEER
FHWA

GENERAL NOTES

ORIENT ANCHOR RODS IN FOOTING AND PROVIDE ANCHOR ROD STICK OUT ABOVE TOP OF CONCRETE FOOTING BASE PER FABRICATION DRAWING.

BENDING DIMENSIONS FOR REINFORCING BARS ARE OUT TO OUT.

USE 3" CLEAR FOR ALL REINFORCEMENT UNLESS NOTED OTHERWISE.

SIGN SUPPORTS SHALL BE LOCATED NORMAL TO ROADWAY.

THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.

WELDING OF ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TEMPLATES SHALL BE USED.

BAR CAGE TO BE ASSEMBLED USING TIE WIRES ONLY, NO WELDING.

BASES (SHAFT) SHALL BE EXCAVATED BY THE USE OF A CIRCULAR AUGER. IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE SOIL, THE FORM SHALL BE REMOVED BEFORE BACK FILLING AROUND THE BASE. ANY REQUIRED BACKFILL SHALL BE WELL COMPACTED IN LAYERS OF 1 FOOT OR LESS. COMPACTION SHALL BE BY MECHANICAL MEANS. CARE SHALL BE TAKEN SO NO DAMAGE OCCURS TO THE CONCRETE BASE DURING COMPACTION.

EXCAVATION OF MATERIALS NOT OCCUPIED BY CONCRETE SHALL BE MINIMIZED TO REDUCE DISTURBANCE OF THE SURROUNDING SOILS.

THE BOTTOM OF THE DRILLED HOLE SHALL BE FIRM AND THOROUGHLY CLEANED SO NO LOOSE OR COMPRESSIBLE MATERIALS ARE PRESENT AT THE TIME OF THE CONCRETE PLACEMENT.

IF THE DRILLED HOLE CONTAINS STANDING WATER, THE CONCRETE SHALL BE PLACED USING A TREMIE TO DISPLACE THE WATER.

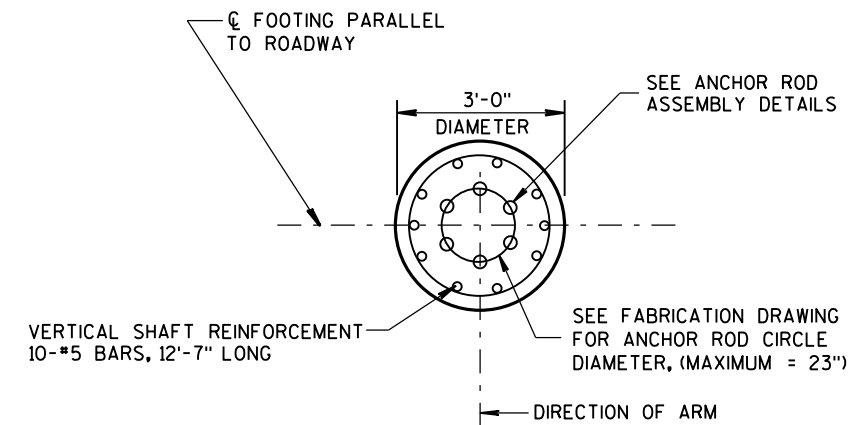
THE REINFORCEMENT AND ANCHOR RODS SHALL BE ADEQUATELY SUPPORTED IN THE PROPER POSITIONS SO NO MOVEMENT OCCURS DURING CONCRETE PLACEMENT.

FORM ALL EXPOSED CONCRETE CORNERS WITH 3/4" CHAMFER ALL AROUND. TOP OF THE CONCRETE BASE SHALL BE TROWEL FINISHED AND LEVEL.

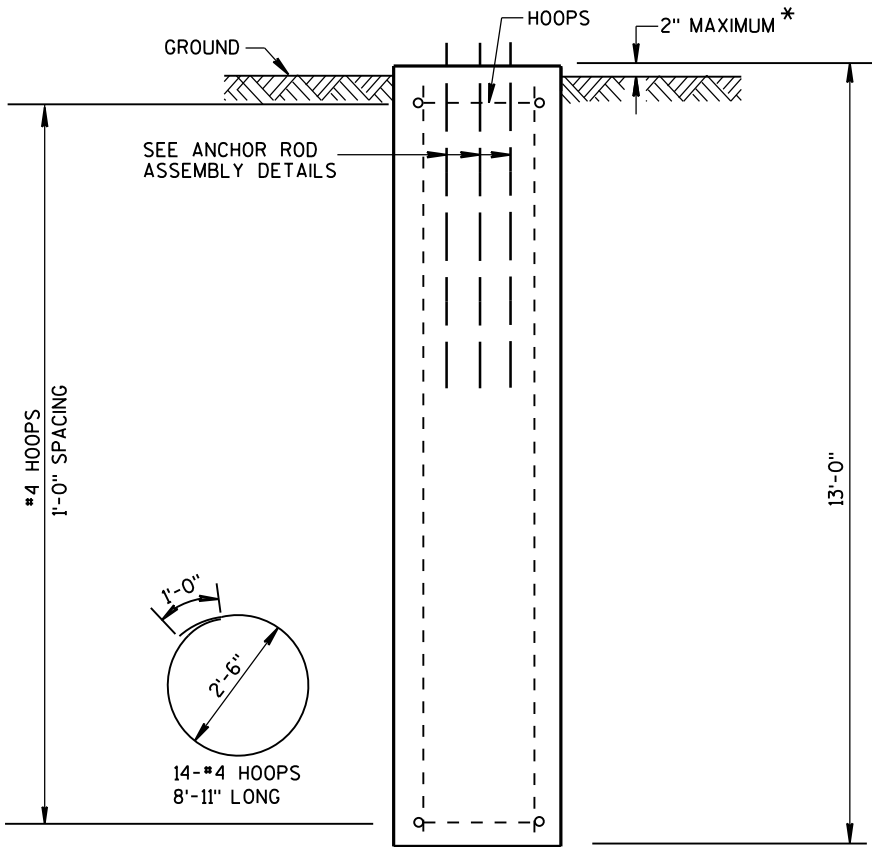
ANY DAMAGE TO THE CONCRETE BASE AND ANCHOR RODS DURING CONSTRUCTION OPERATIONS SHALL BE REPAIRED AT THE ENGINEER'S DIRECTION, AT THE EXPENSE OF THE CONTRACTOR.

CONCRETE MASONRY ----- $f_c=3,500$ p.s.i.
HIGH STRENGTH BAR STEEL REINFORCEMENT, GRADE 60 ----- $f_y=60,000$ p.s.i.
ANCHOR RODS, ASTM F1554, GRADE 55 ----- $f_y=55,000$ p.s.i.
ASTM A563A HEAVY HEX NUTS, AND ASTM F436 WASHERS.
PLATES, ASTM A709, GRADE 36 ----- $f_y=36,000$ p.s.i.

THIS FOOTING HAS BEEN DESIGNED FOR SITES WHERE SOILS EXHIBIT A PHI-ANGLE GREATER THAN OR EQUAL TO 20 DEGREES (GRANULAR SOILS), OR A COHESION VALUE GREATER THAN OR EQUAL TO 350 PSF (COHESIVE SOILS).



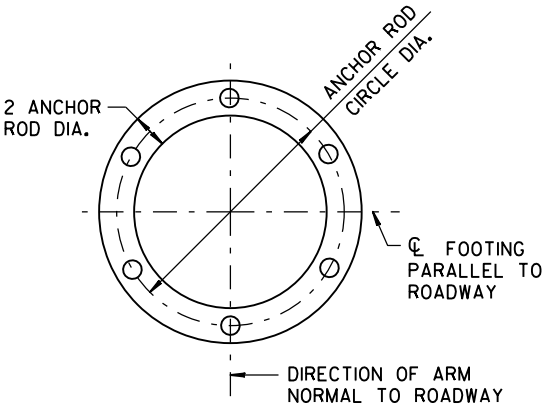
PLAN VIEW



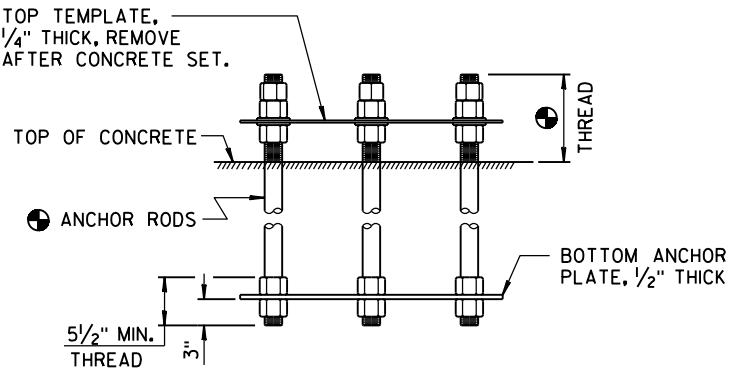
ELEVATION VIEW

* FOR OVERHEAD SIGN SUPPORTS THAT ARE INSTALLED ADJACENT TO SIDEWALKS, THE TOP OF THE BASE SHALL BE POURED FLUSH WITH THE GROUND.

CONCRETE - 3.4 C.Y. PER FOOTING
H.S. REINFORCEMENT - 215 LBS. PER FOOTING



TOP TEMPLATE AND
BOTTOM ANCHOR PLATE

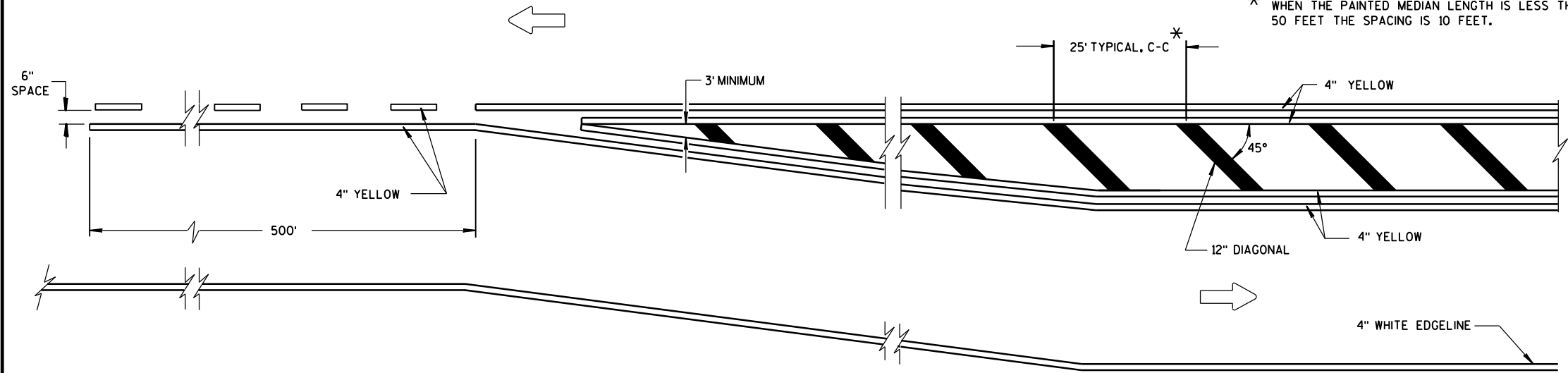


MINIMUM OF 6 ANCHOR RODS, EXACT NUMBER, SIZE, DIMENSION AND ORIENTATION AS SHOWN ON FABRICATION DRAWING.

36" DIAMETER FULL SPAN
OVERHEAD SIGN SUPPORT BASE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016 /S/ Vu Thao
DATE WIND LOADED STRUCTURES
FHWA PROGRAM LEADER

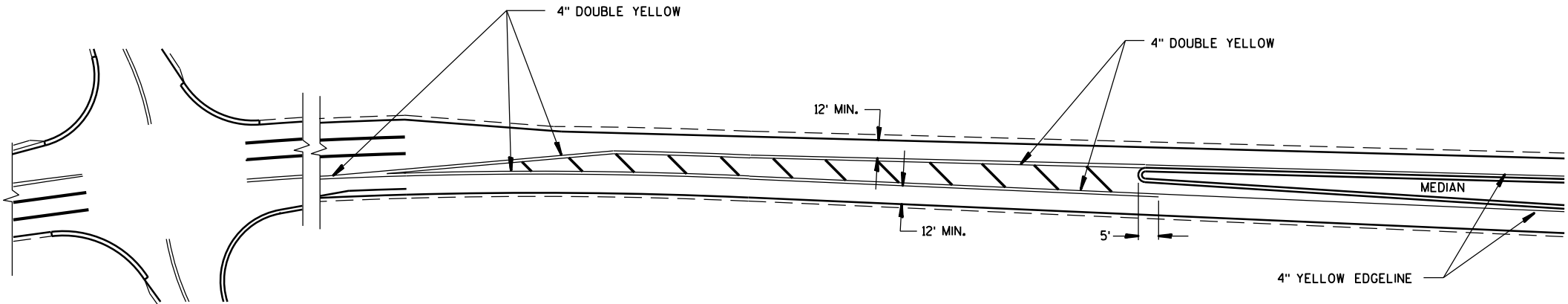


MEDIAN ISLAND DETAIL

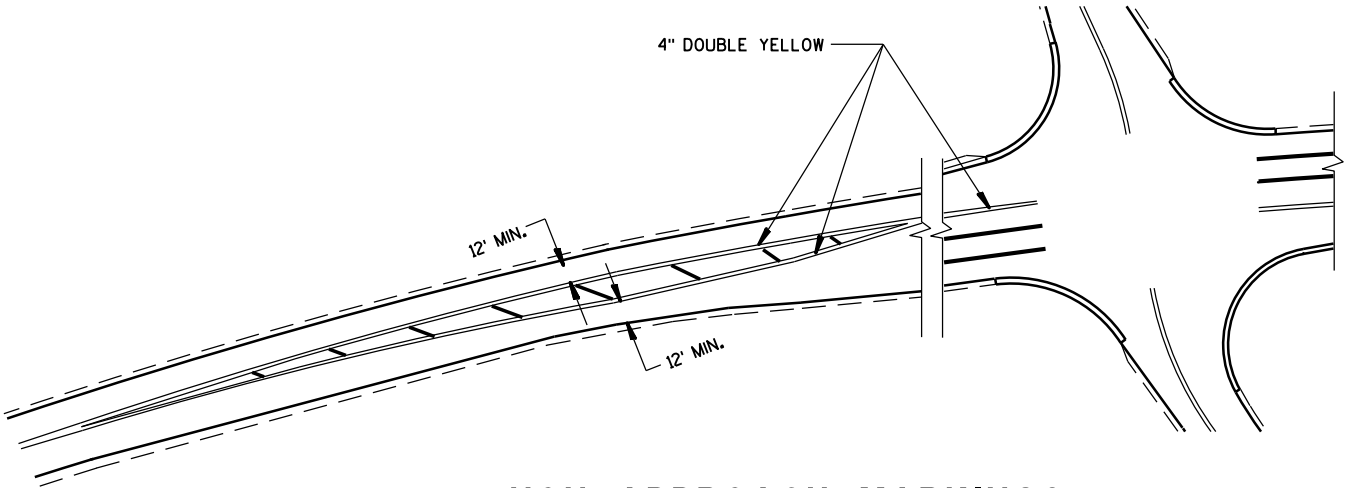
GENERAL NOTE

DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT WIDEST POINT.

➡ DIRECTION OF TRAVEL

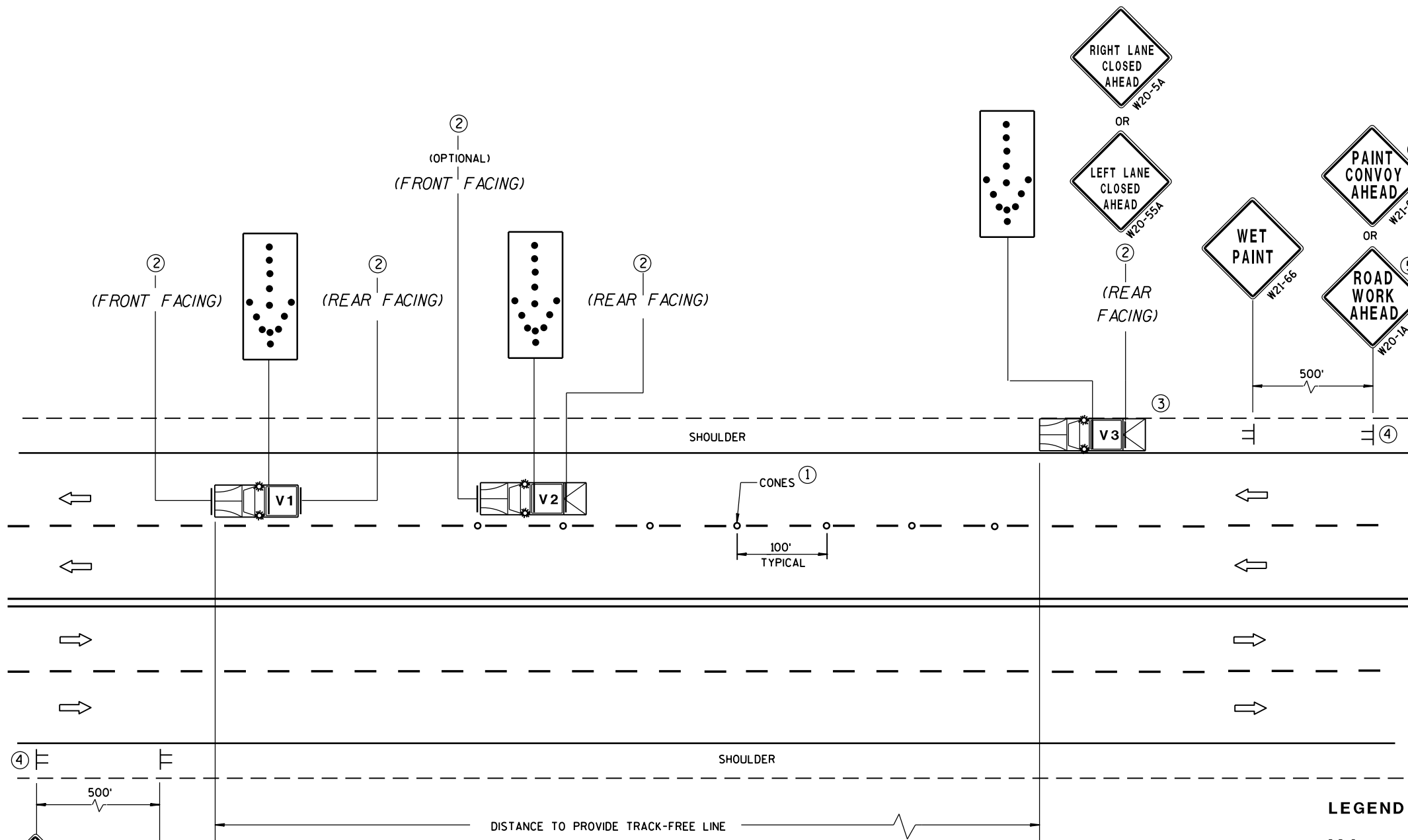


APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON APPROACH MARKINGS

MEDIAN ISLAND MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	



MOVING PAVEMENT MARKING OPERATIONS
MULTI-LANE UNDIVIDED ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

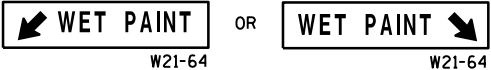
FOR EDGE LINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGE LINE OR LANELINE MARKING FOR MULTILANE UNDIVIDED ROADWAYS.

① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.



③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.

④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

V1 LEAD VEHICLE

V2 SHADOW VEHICLE

V3 TRAIL VEHICLE WITH TMA

TMA TRUCK-MOUNTED ATTENUATOR

Sign on temporary support

Direction of traffic

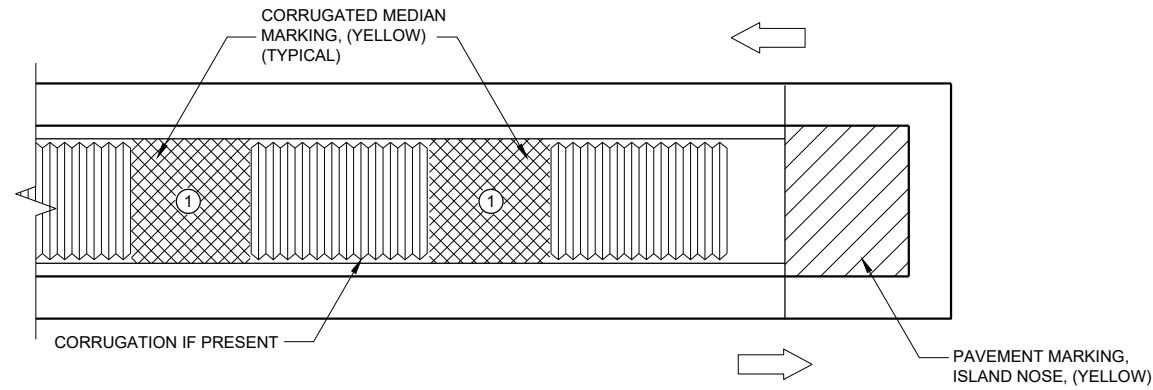
Cones

Flashing arrow panel (merge)

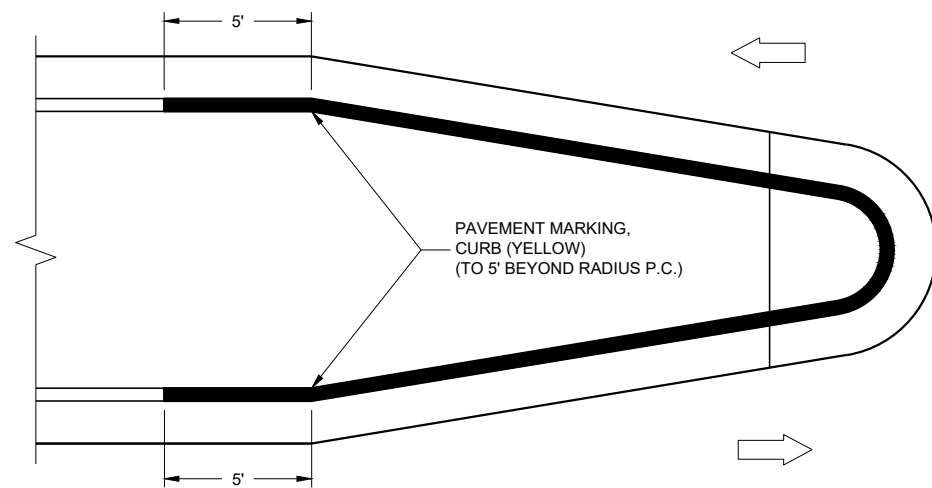
MOVING PAVEMENT MARKING
OPERATION
MULTI-LANE UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

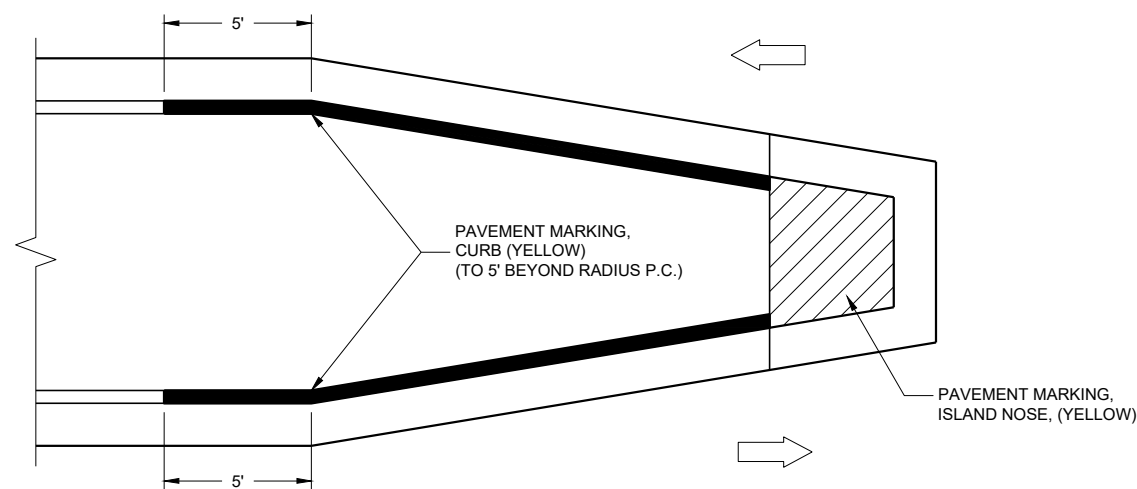
APPROVED
Sept., 2017 /S/ Andrew Heldtke
DATE WORK ZONE ENGINEER
FHWA



MEDIAN ISLAND WITH SQUARE BLUNT NOSE







MEDIAN ISLAND WITH ROUND BLUNT NOSE



TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

-  ISLAND NOSE MARKING
-  CURB MARKING
-  CORRUGATED MEDIAN MARKING
-  DIRECTION OF TRAVEL

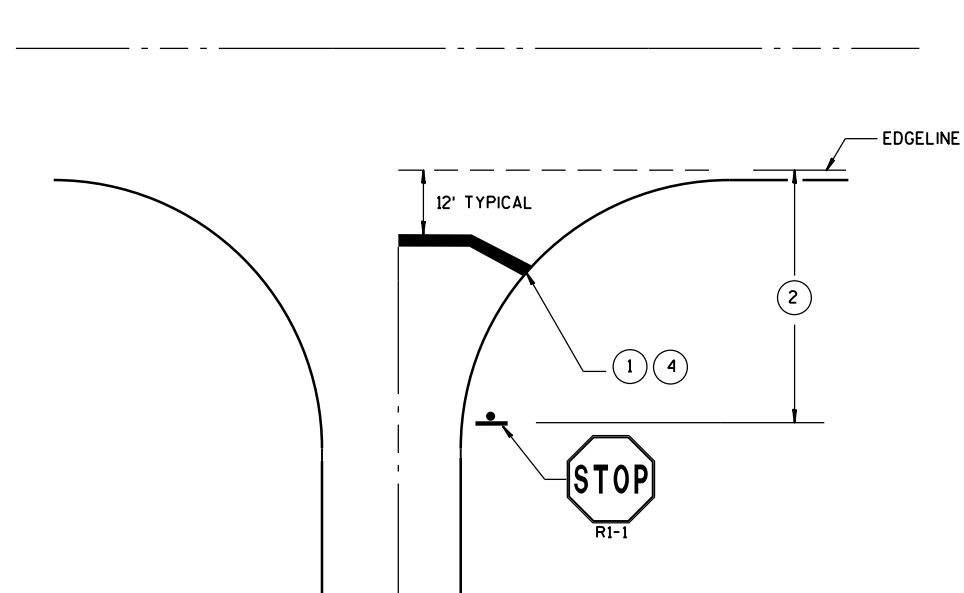
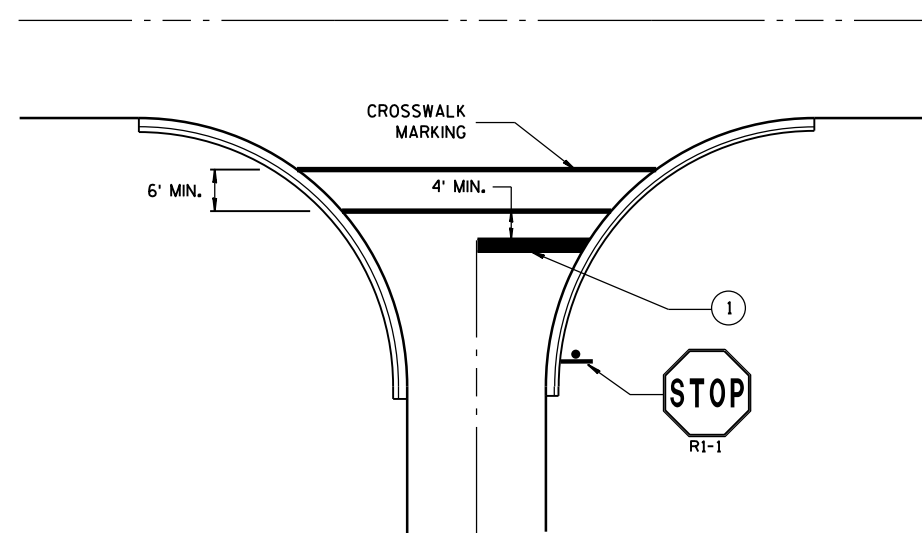
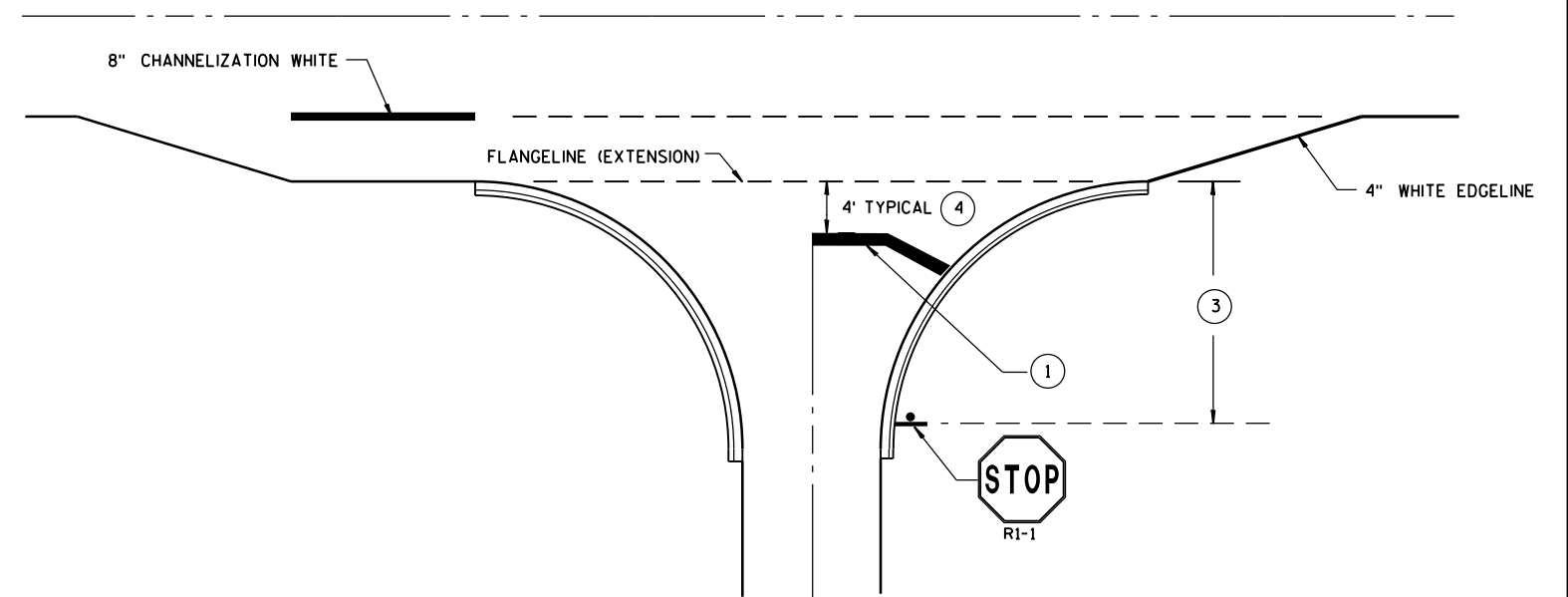
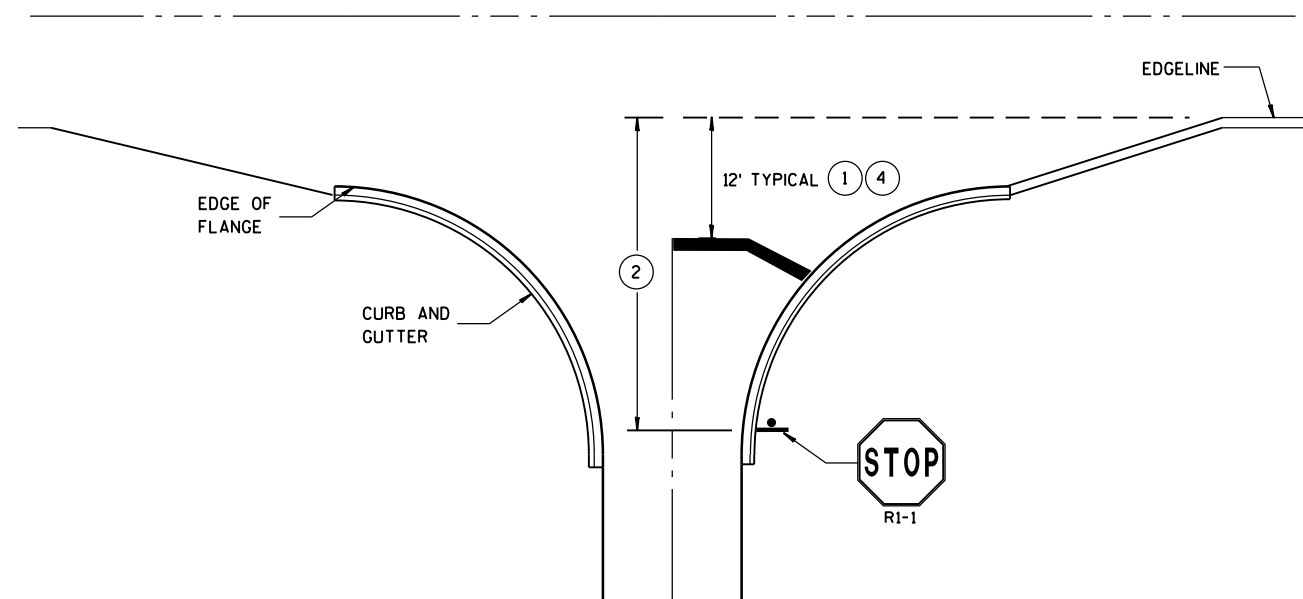
PAVEMENT MARKINGS (ISLANDS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MARKING
ENGINEER

FHWA



GENERAL NOTES

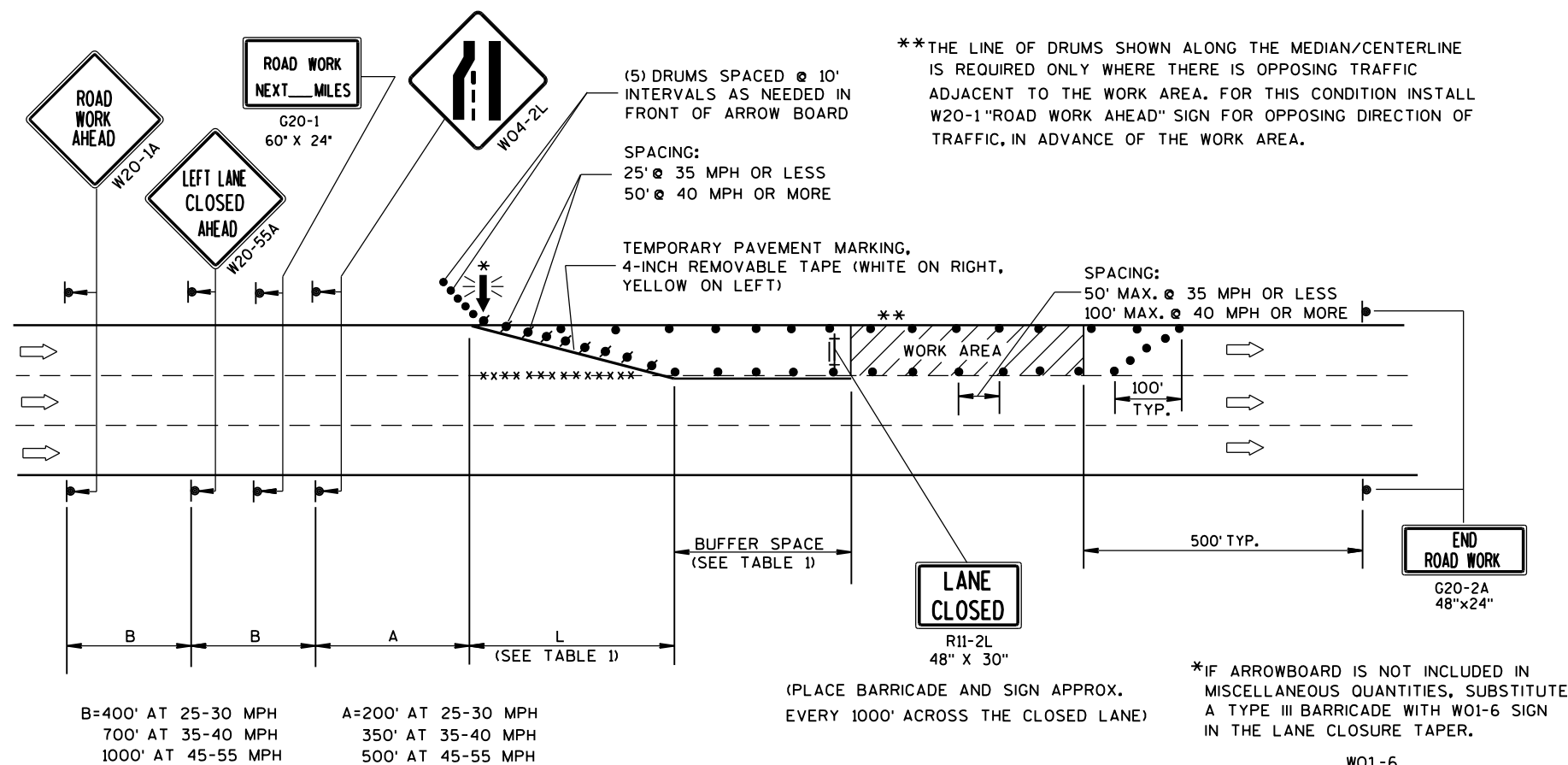
STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

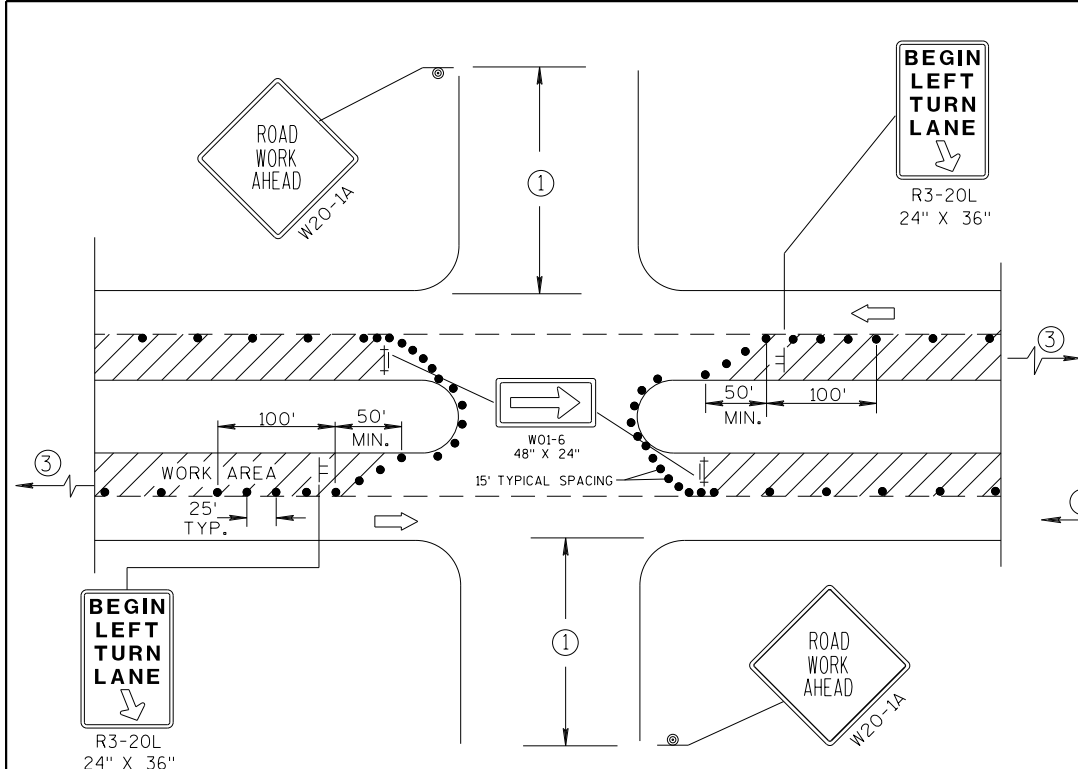
- 1 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- 2 IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
- 3 IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- 4 MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

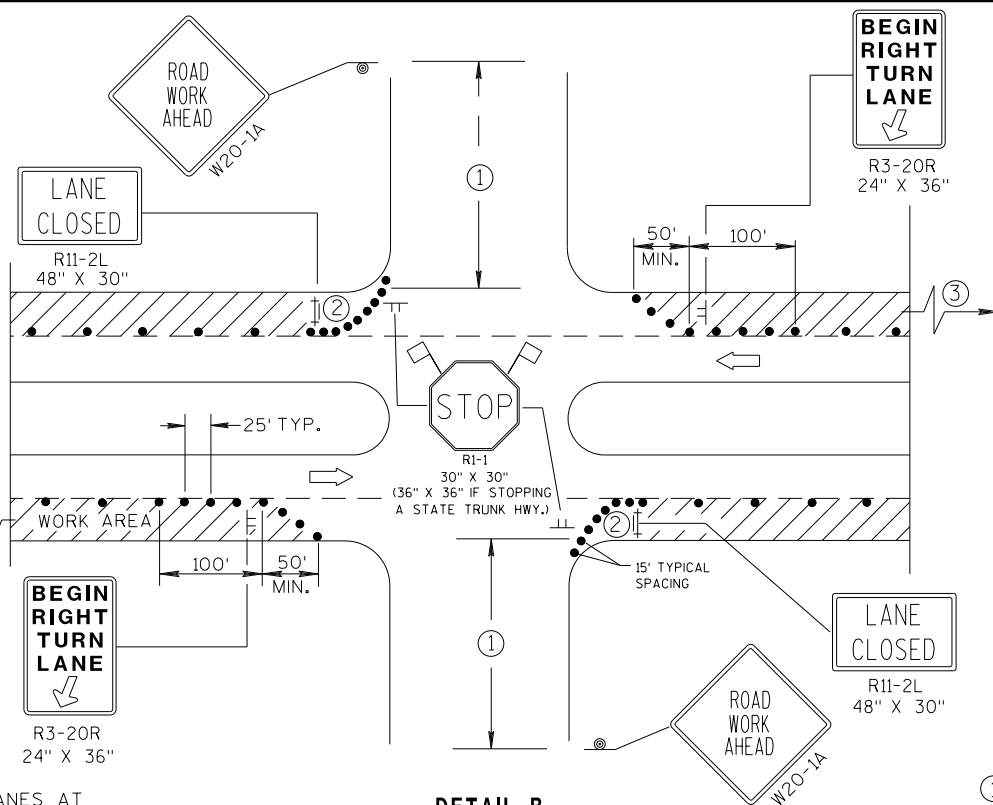
APPROVED
Sept., 2017 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FWWA





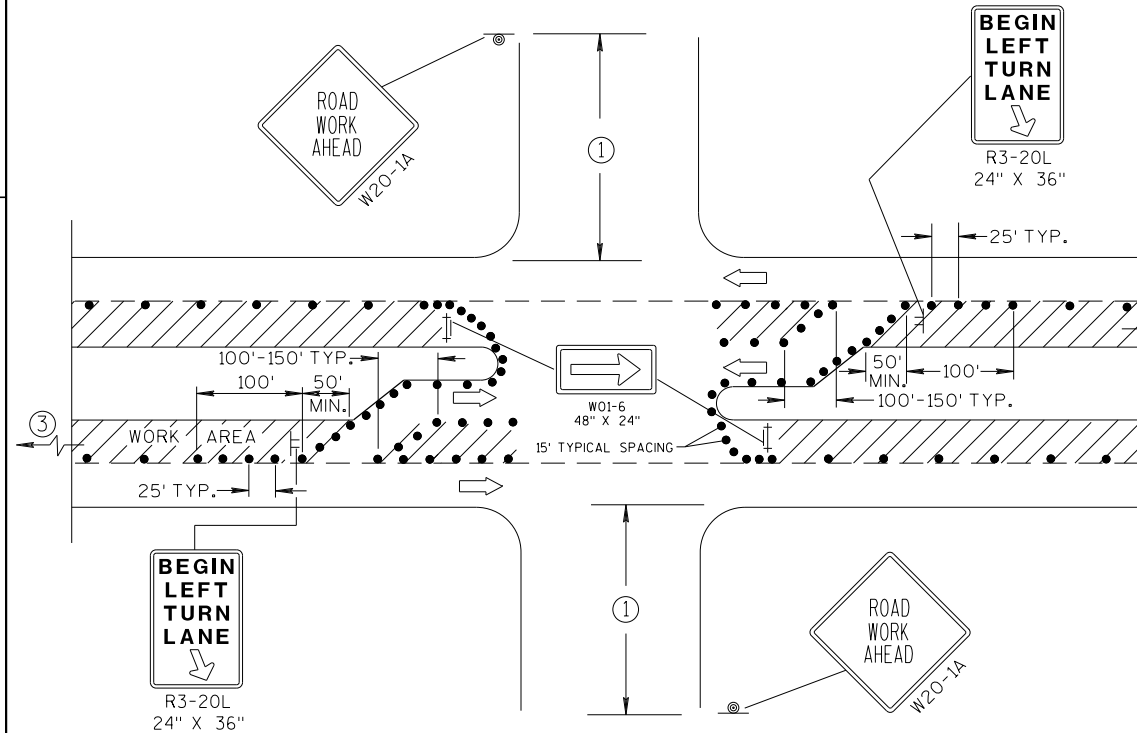
DETAIL A
FOR LEFT LANE CLOSURE AT
INTERSECTION OR MEDIAN OPENING

PROVIDE TURN LANES AT
INTERSECTIONS WHENEVER
STAGING OF WORK ALLOWS.
TAPER AND TURN LANE
LENGTHS BASED ON FIELD
CONDITIONS AS APPROVED
BY THE ENGINEER.

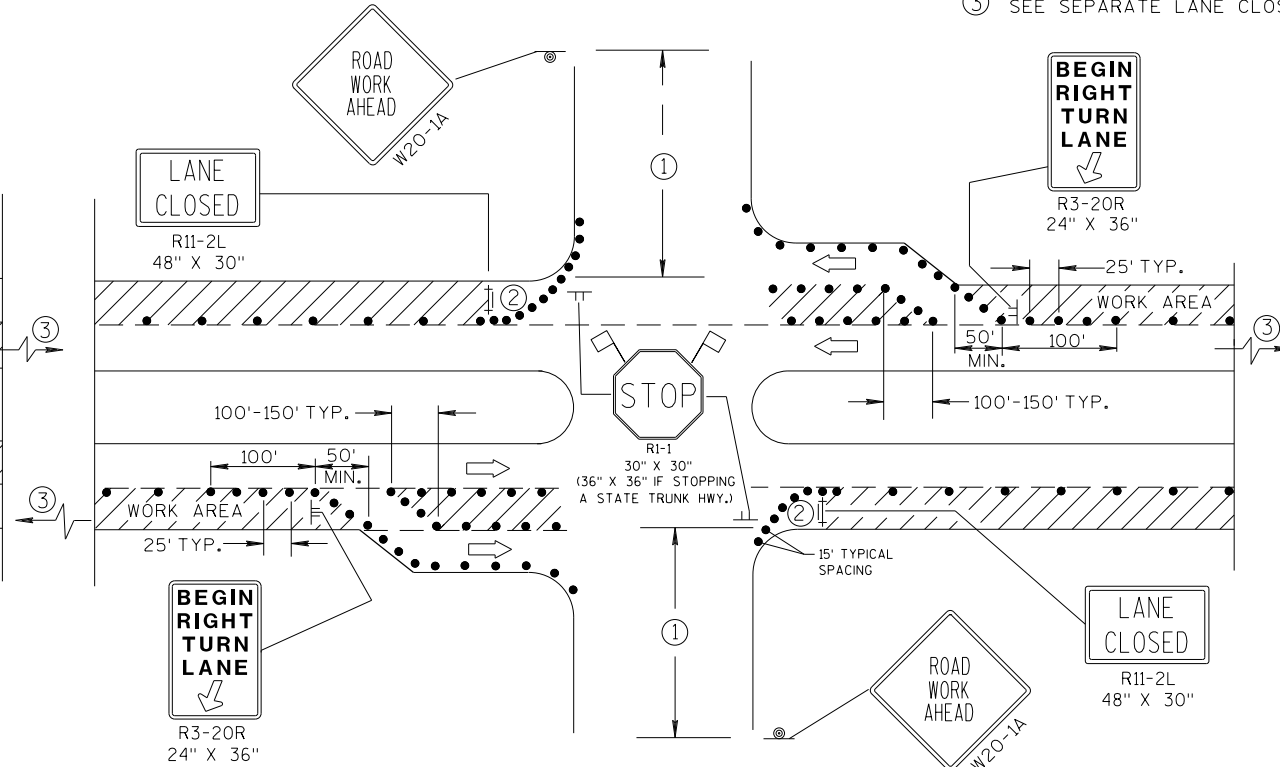


DETAIL B
FOR RIGHT LANE CLOSURE
AT INTERSECTION

- ① 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35-40 MPH.
200' IF 25-30 MPH.
- ② ALSO USE BARRICADE AND 15-FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS.
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.



DETAIL C
FOR LEFT LANE CLOSURE AT INTERSECTION OR
MEDIAN OPENING (WITH LEFT TURN BAY OPEN)



DETAIL D
FOR RIGHT LANE CLOSURE AT INTERSECTION
(WITH RIGHT TURN BAY OPEN)

LEGEND

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ≡ SIGN ON TEMPORARY SUPPORT (5' MIN. MOUNTING HEIGHT)
- ⌄ TYPE III BARRICADE WITH ATTACHED SIGN AND TYPE "A" WARNING LIGHT (FLASHING)
- ➡ DIRECTION OF TRAFFIC
- 🚩 FLAGS, 16" X 16" MIN., (ORANGE)
- ▨ WORK AREA

TRAFFIC CONTROL,
INTERSECTION WITHIN
SINGLE LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

TABLE A

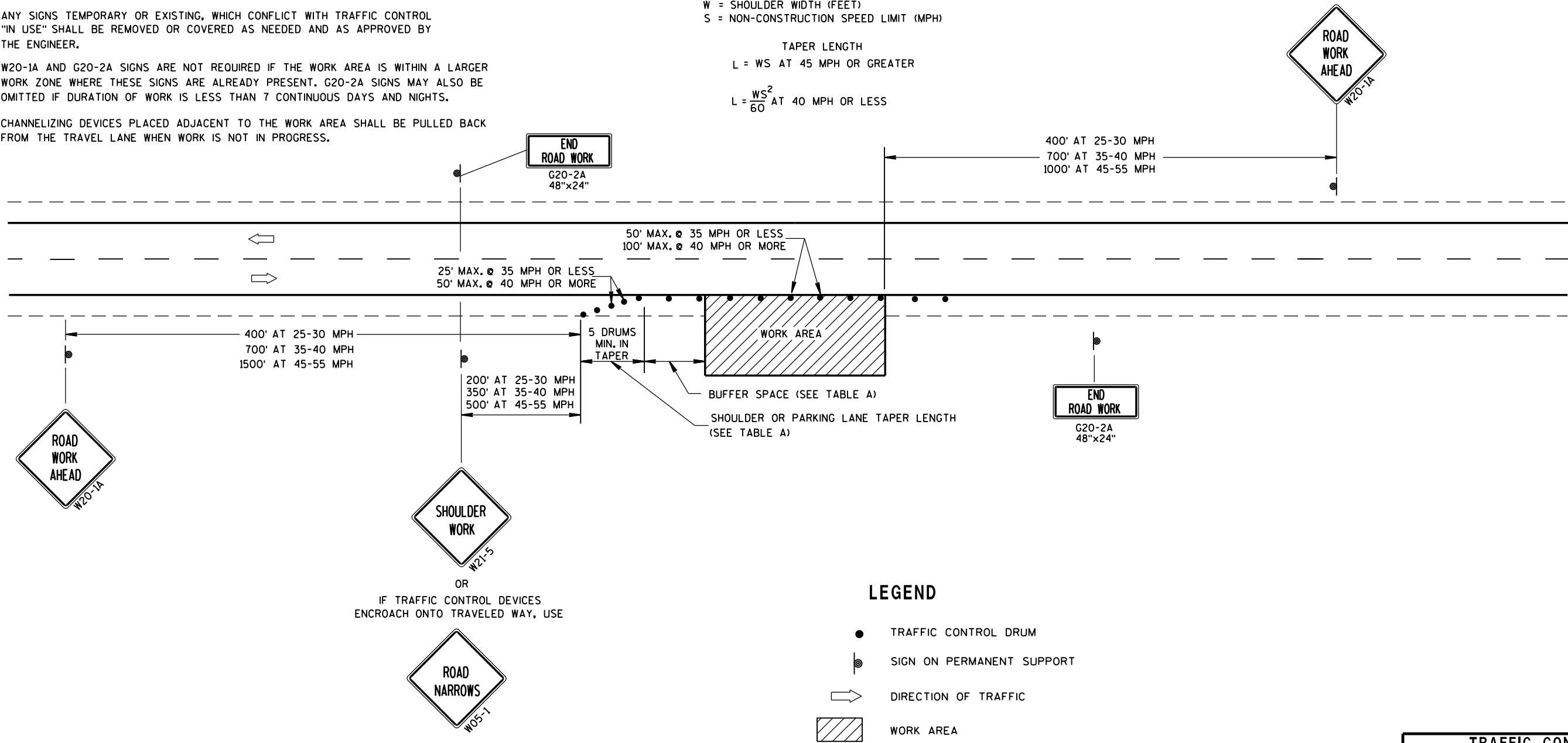
SHOULDER TAPER LENGTH (FEET)					BUFFER SPACE (FEET)
S \ W	4	6	8	10	
30	20	30	40	50	200
35	30	45	55	70	250
40	40	55	75	90	305
45	60	90	120	150	360
50	70	100	135	170	425
55	75	110	150	185	495

W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH
L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = $\frac{1}{3}L$

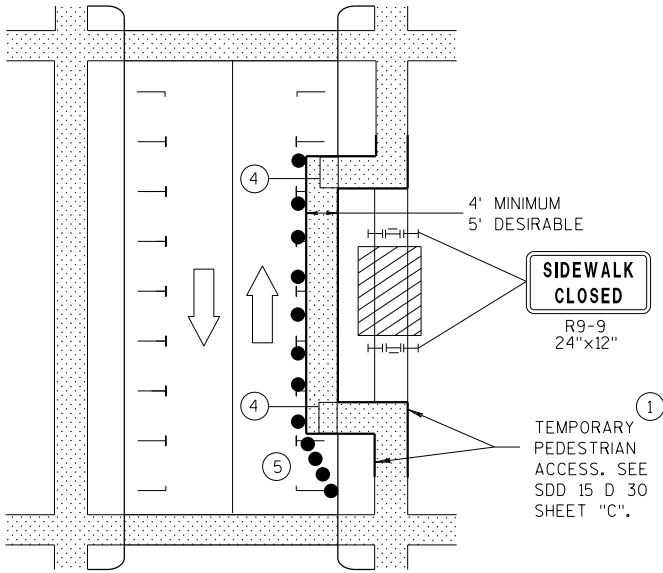


LEGEND

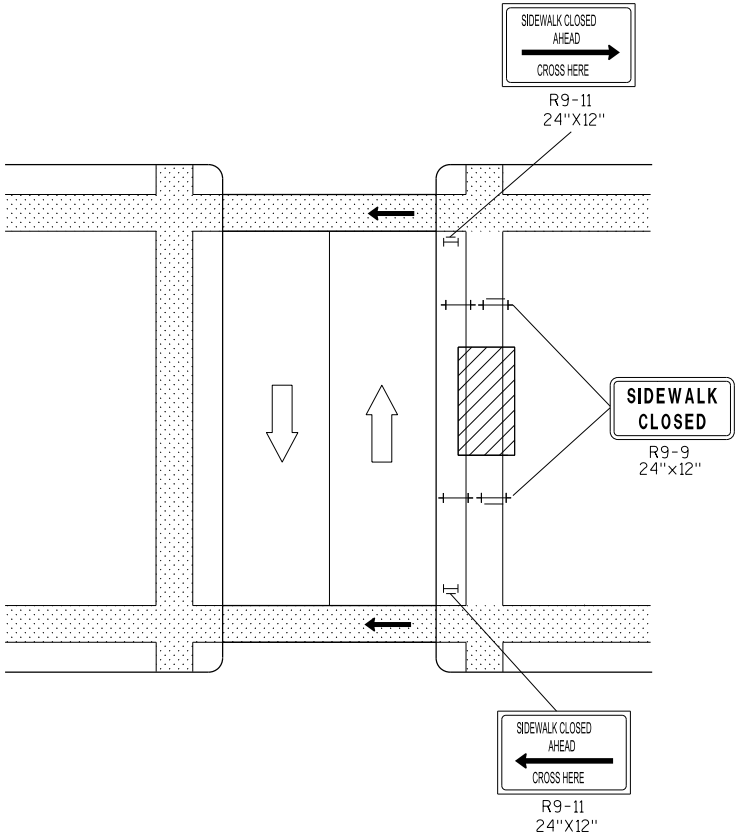
- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 14, 2015 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

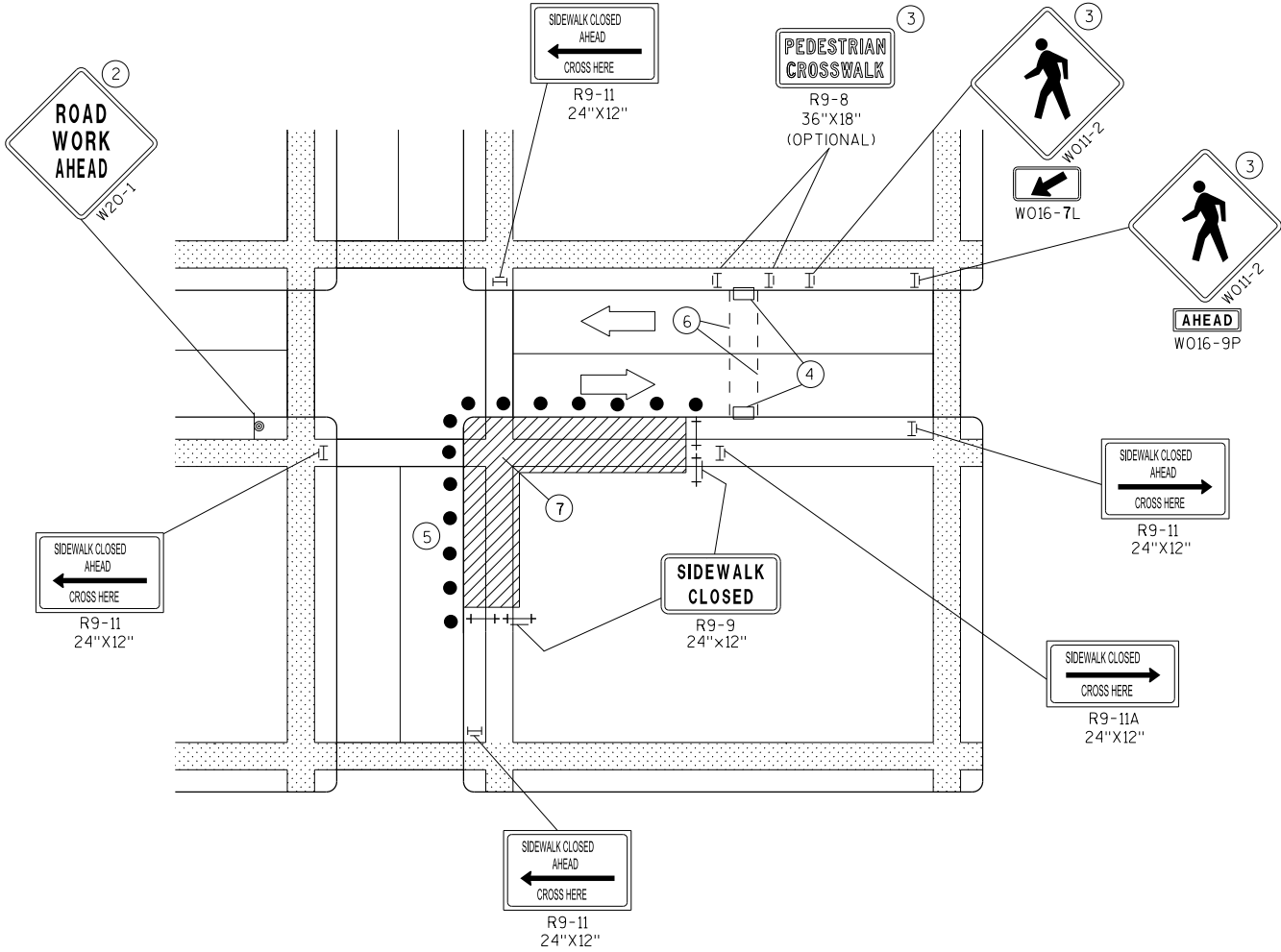
NOTE: MAY BE USED ON ROADWAY WITH POSTED SPEED OF LESS THAN 40 MPH.



MID-BLOCK SIDEWALK CLOSURE
IN PARKING LANE

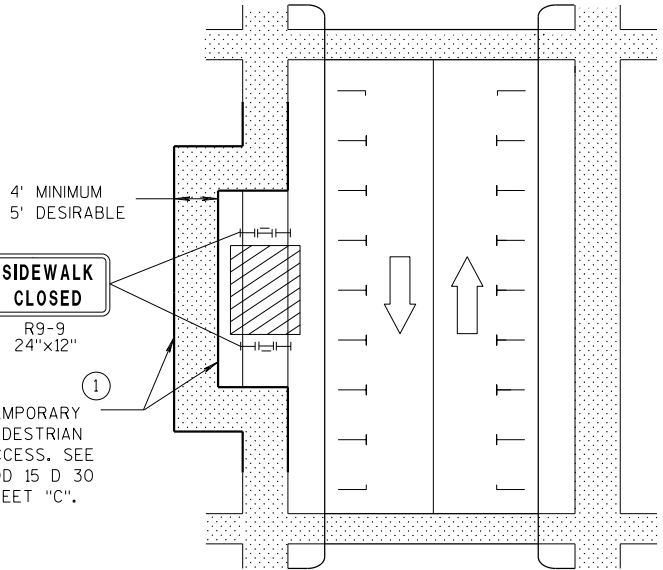


MID-BLOCK SIDEWALK CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

NOTE: LAYOUT SAME AS ABOVE.



SIDEWALK DIVERSION

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.
- "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND WO11-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.
- TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".
- DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

LEGEND

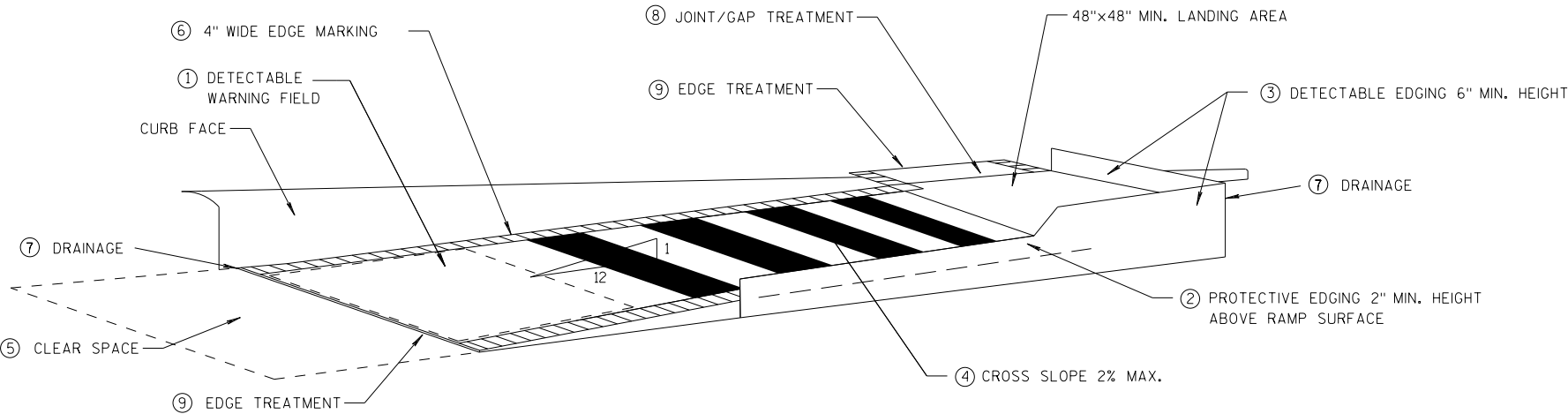
	SIGN ON PERMANENT SUPPORT		DIRECTION OF TRAFFIC
	UNDER PEDESTRIAN TRAFFIC		TRAFFIC CONTROL DRUM
	WORK AREA		
	PEDESTRIAN CHANNELIZATION DEVICE		
	TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		
	TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

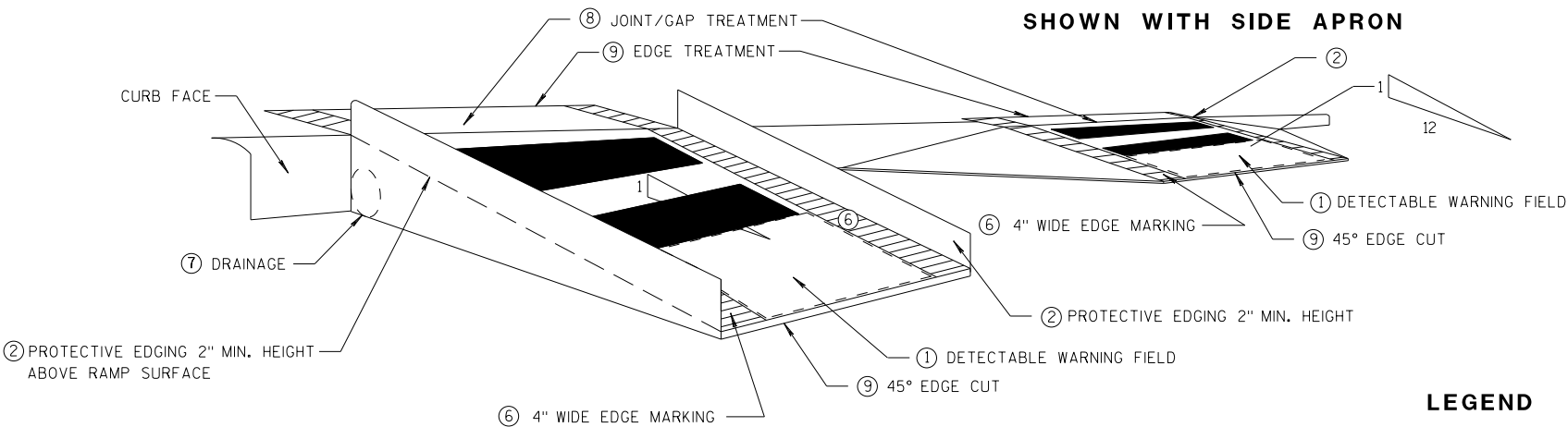
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.
- 1 CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 805 SHEET "E".
 - 2 PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
 - 3 DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
 - 4 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 - 5 CLEAR SPACE OF 48"x48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 - 6 THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
 - 7 DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
 - 8 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 - 9 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
 - 10 5' WIDE MIN. WITH PEDSETRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.

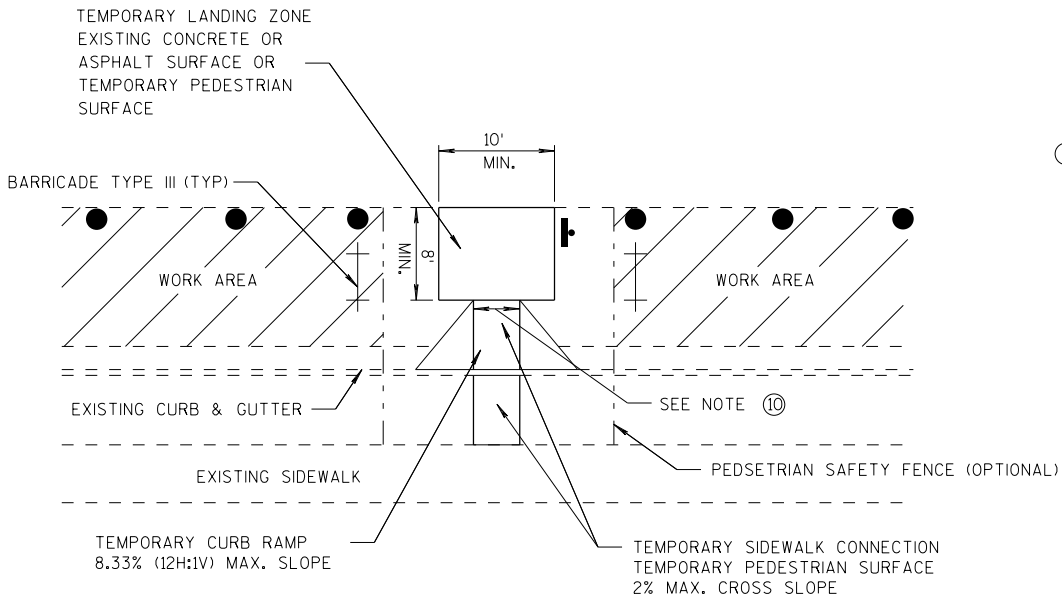


TEMPORARY CURB RAMP
PARALLEL TO CURB



SHOWN WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP
PERPENDICULAR TO CURB



TEMPORARY BUS STOP PAD

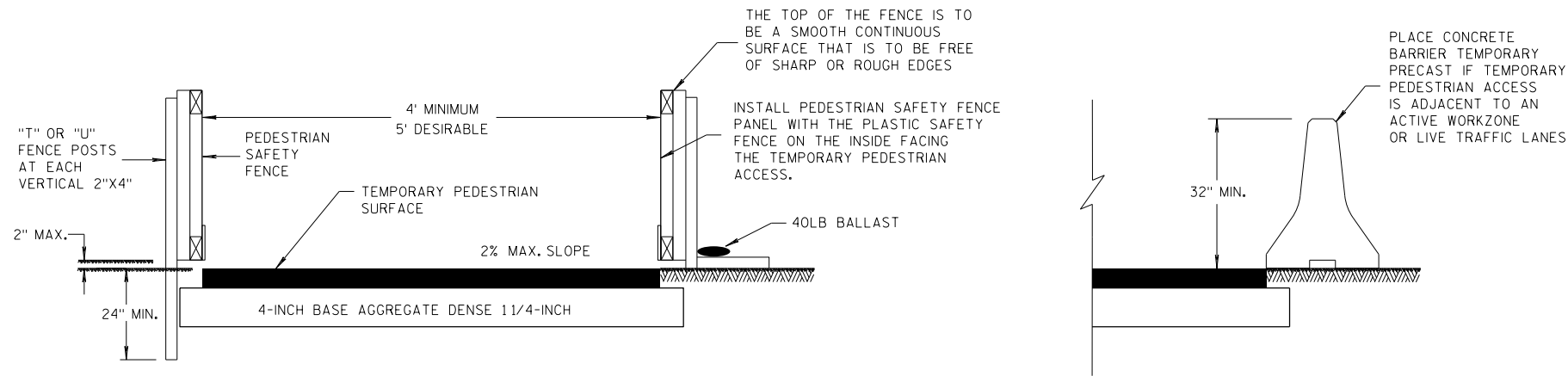
LEGEND

- WORK AREA
- TYPE III BARRICADE
- TRAFFIC CONTROL DRUM

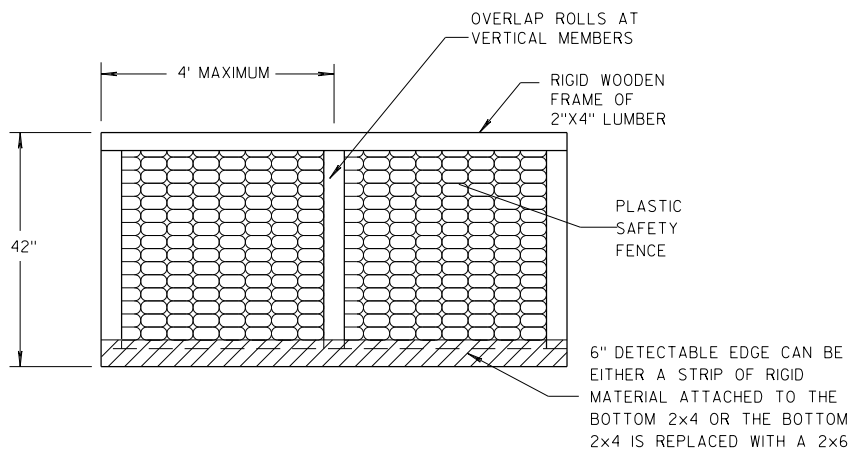
TRAFFIC CONTROL,
TEMPORARY ADA COMPLIANT
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

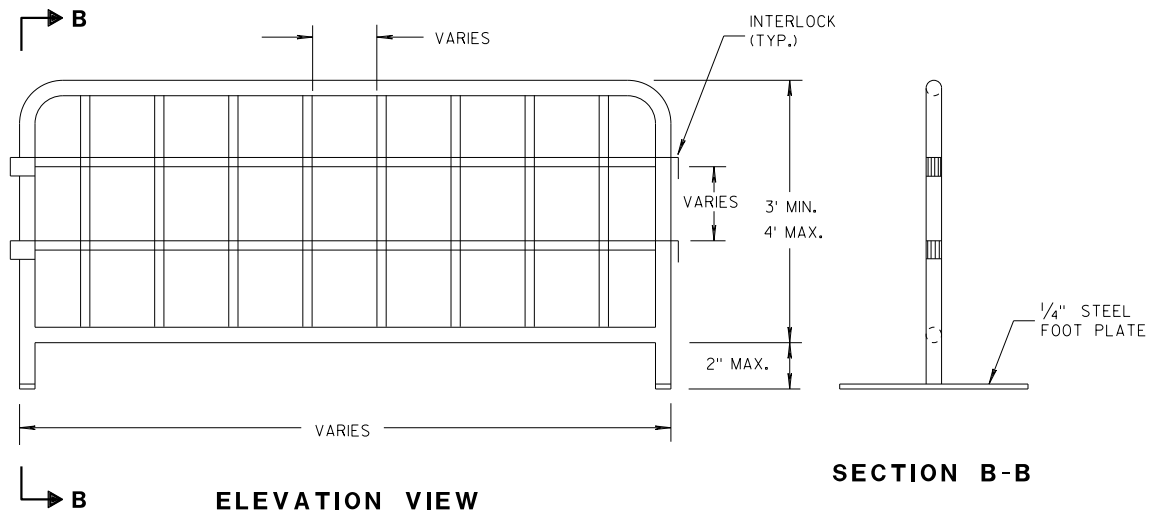
APPROVED
DATE 7/2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER
FHWA



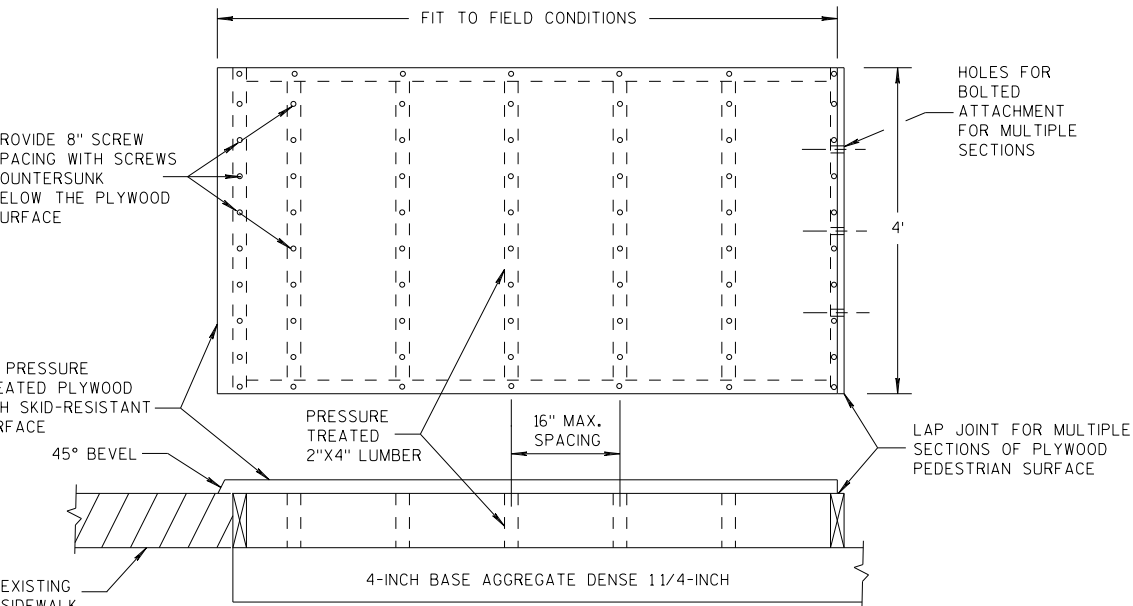
TEMPORARY PEDESTRIAN ACCESS



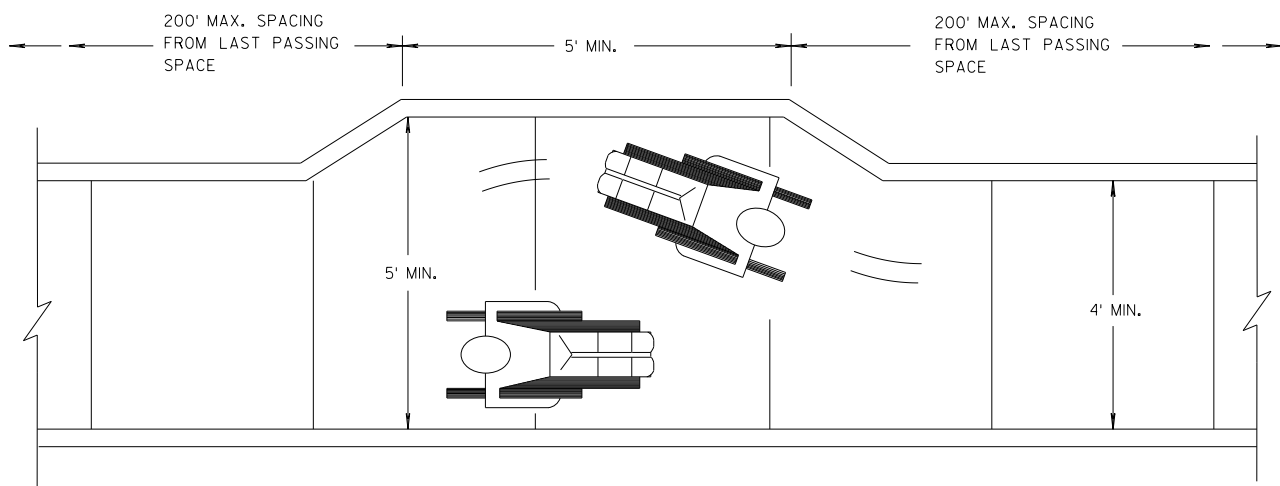
PEDESTRIAN SAFETY FENCE



TEMPORARY PEDESTRIAN STEEL BARRICADE

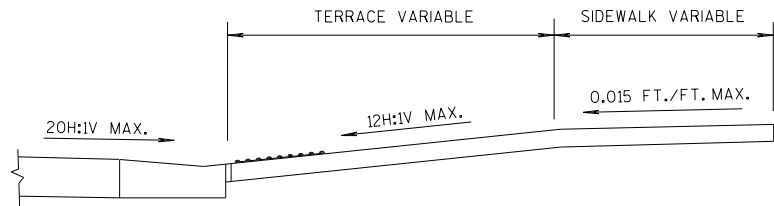


TEMPORARY PEDESTRIAN SURFACE PLYWOOD

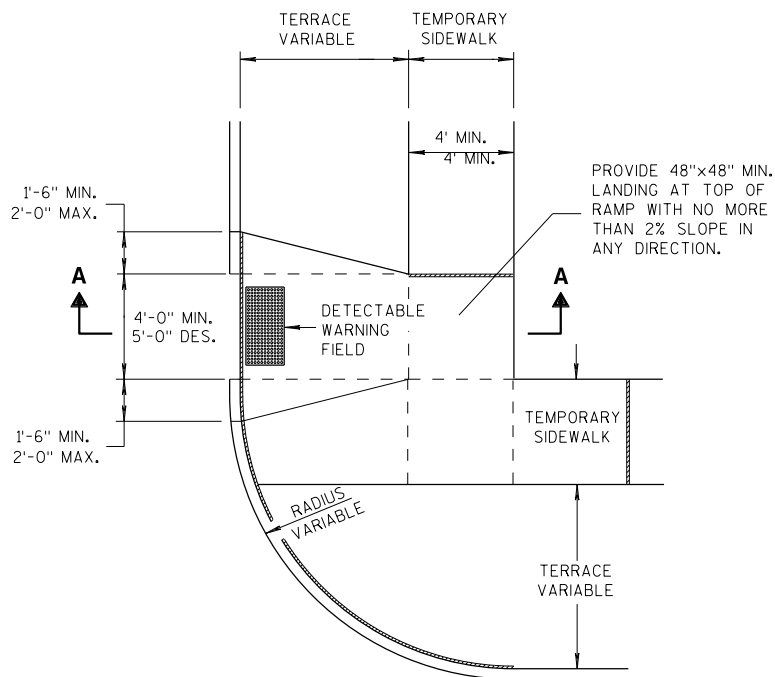


NARROW SIDEWALK PASSING DETAIL

- GENERAL NOTES**
- ① INTERCHANGEABLE WITH THE PEDESTRIAN SAFETY FENCE.



SECTION A-A



PLAN VIEW
TEMPORARY TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



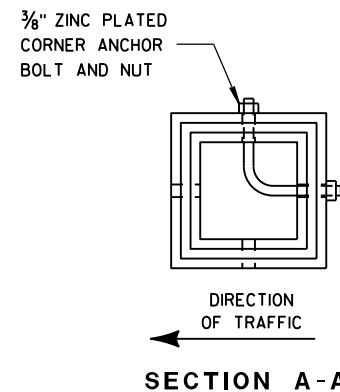
DETAIL OF TUBULAR
STEEL SIGN POST

TUBULAR STEEL POSTS

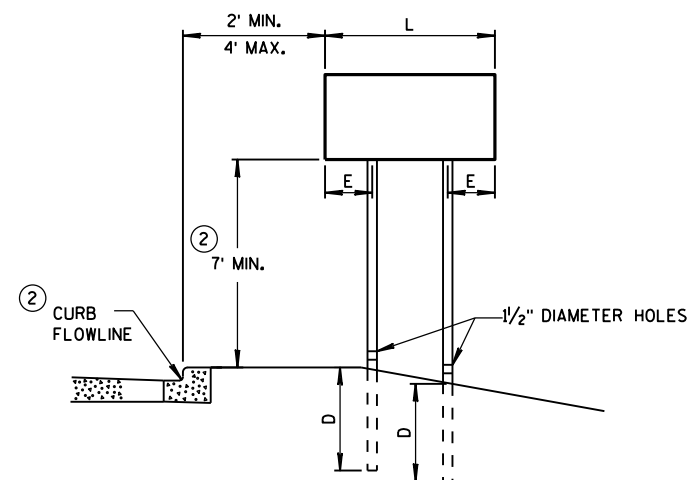
AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL
BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED
ON TUBULAR STEEL POSTS.



SECTION A-A

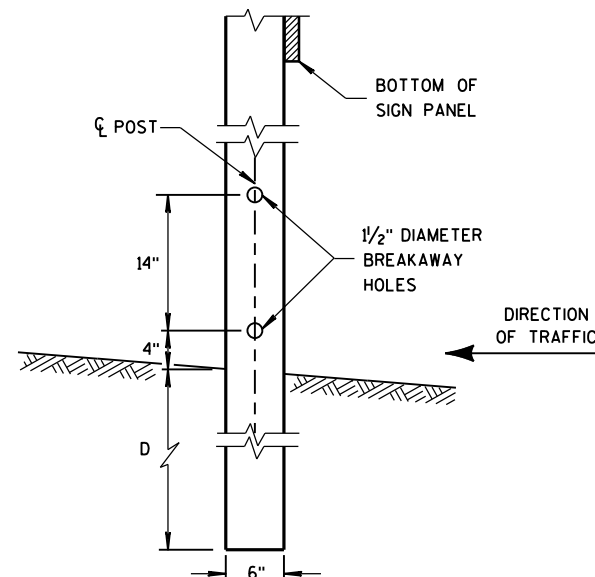


URBAN AREA

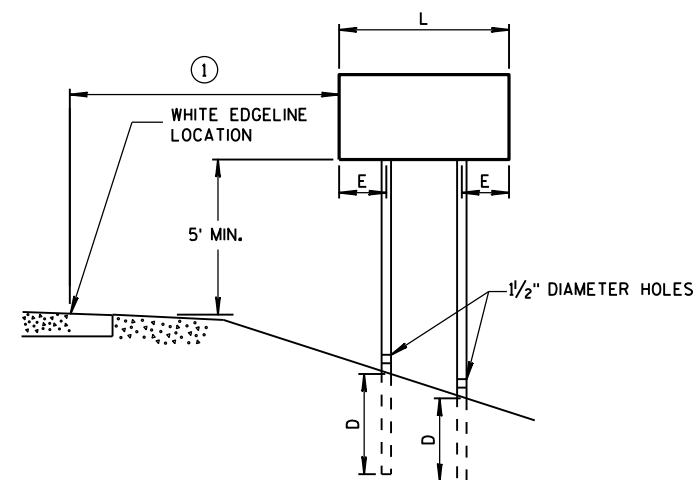
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST
EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



4 "x6 " WOOD POST
MODIFICATION



RURAL AREA

4 " X 6 " WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL
SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" x 3"
 - MACHINE BOLTS - 5/16" x 6-1/2" OR 7" LENGTH W/ NUTS

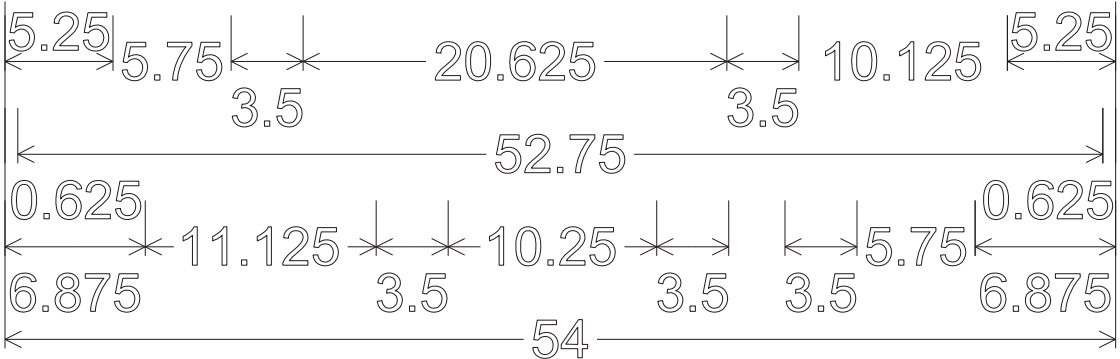
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" x 3-1/4" LENGTH W/ NUTS
 - RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

- WASHERS (ALL POSTS) -
- 1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL
 - 1-1/4" O.D. x 3/8" I.D. x .080 NYLON FOR ALL TYPE H SIGNS

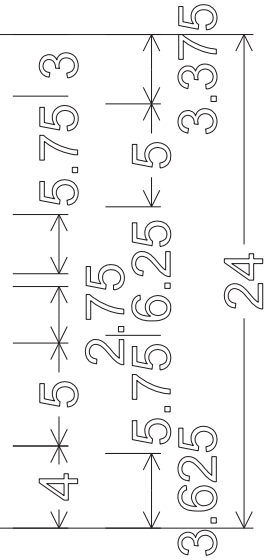
* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

1-1



1.250" Radius, 0.625" Border



NOTES

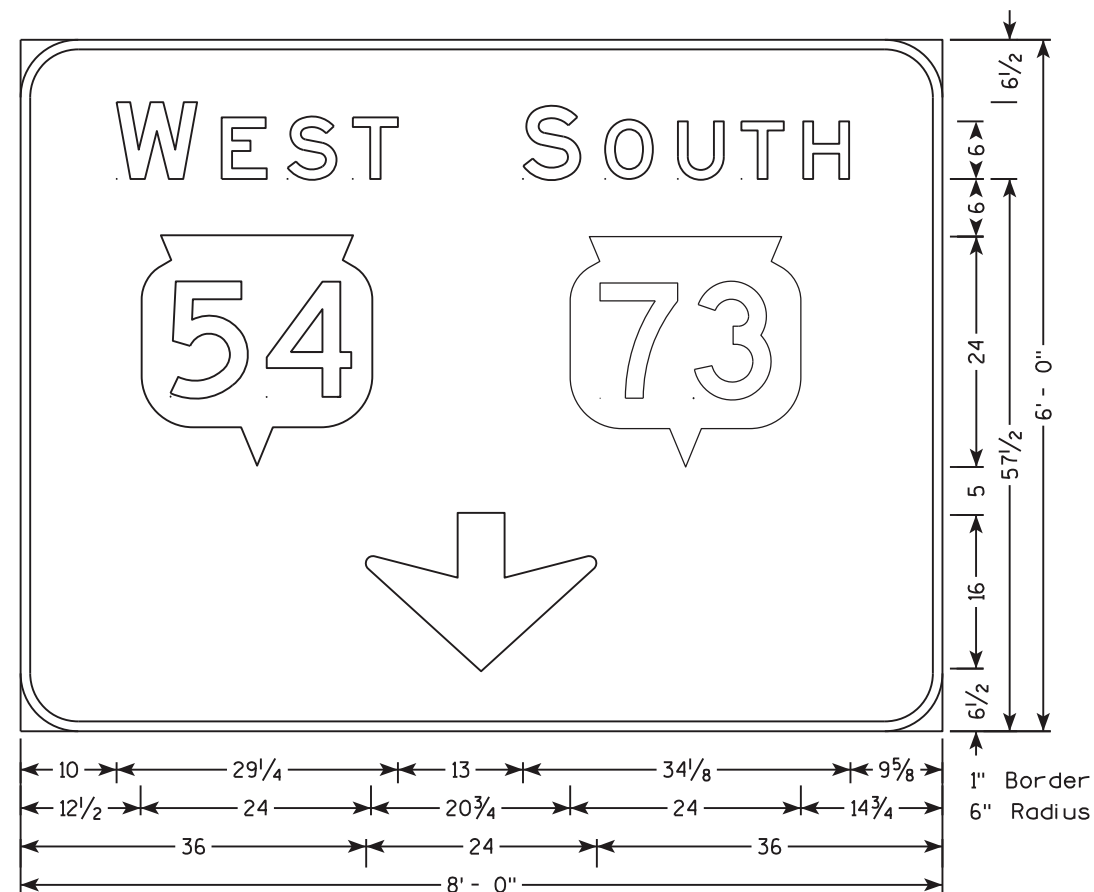
1. All Signs Type II - Type H Reflective
2. Color:
Background - GREEN
Message - WHITE
3. Message Series - C

7

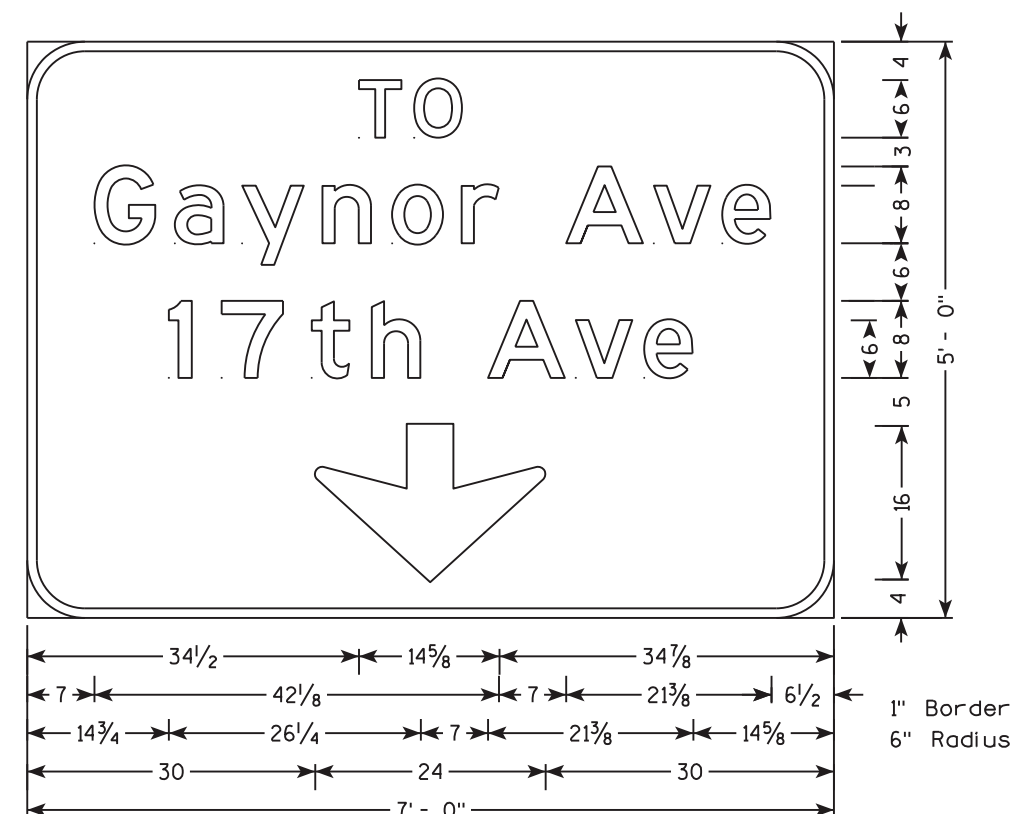
7

NOTES

1. All Signs are Type I - Type SH Reflective
2. Color:
Background - Green
Message - White
3. Message Series - E



E6-51

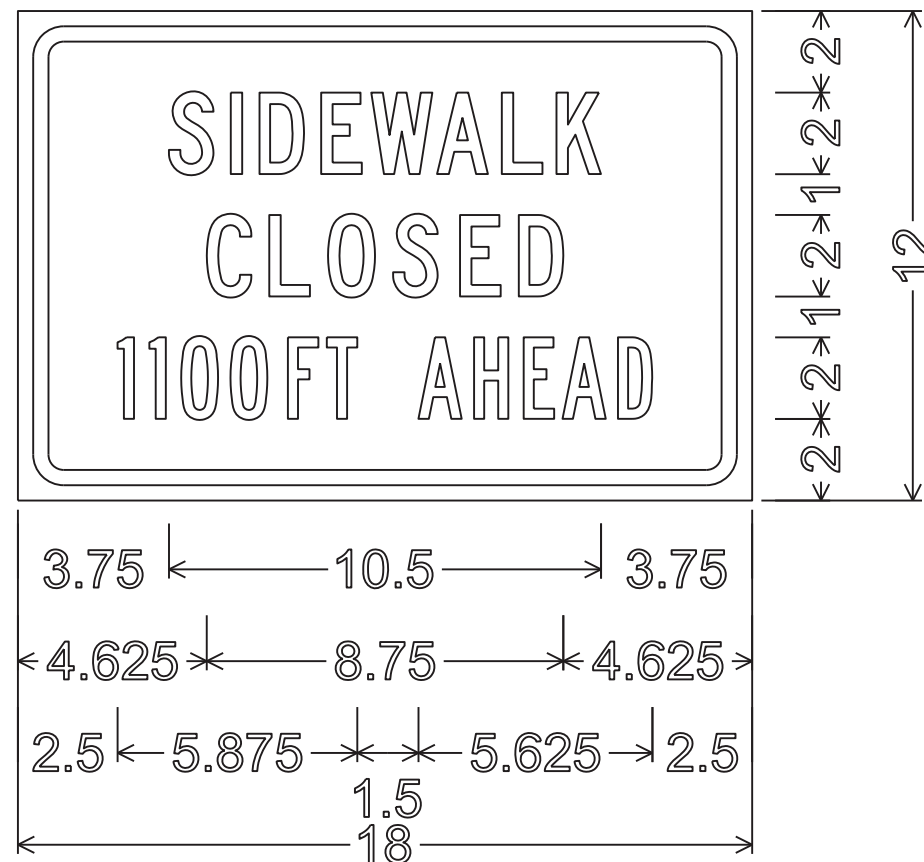


E6-51



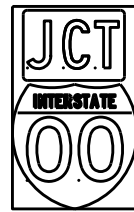
NOTES

1. Fixed Message Type II Signs - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
3. Message Series - As noted

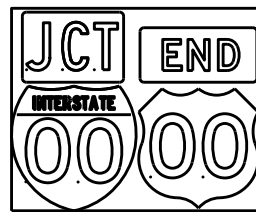


1.125" Radius, 0.375" Border, 0.375" Indent,
 "SIDEWALK" C; "CLOSED" C; "1100FT" B; "AHEAD" B

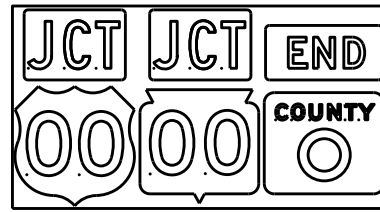
TYPICAL ASSEMBLIES



J1-1



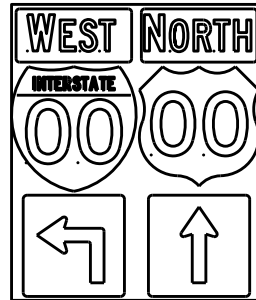
J1-2



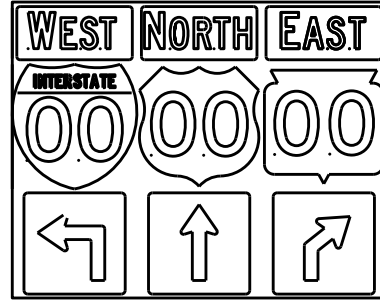
J1-3



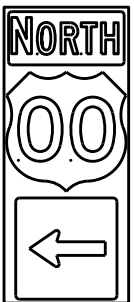
J2-1



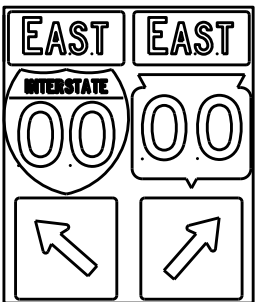
J2-2



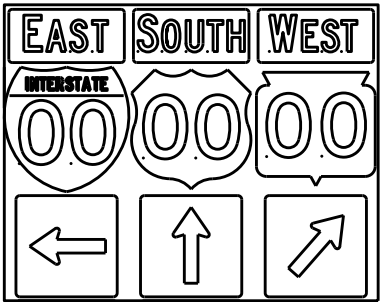
J2-3



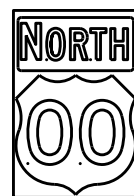
J3-1



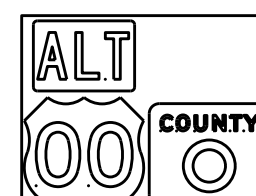
J3-2



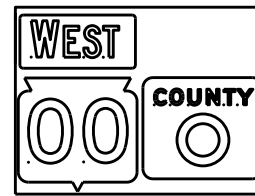
J3-3



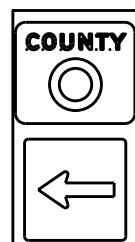
J4-1



J4-2



J4-2



J13-1



J12-1



J32-1



J33-1



J23-1

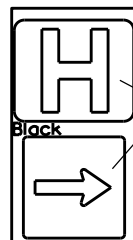


J22-1



JV

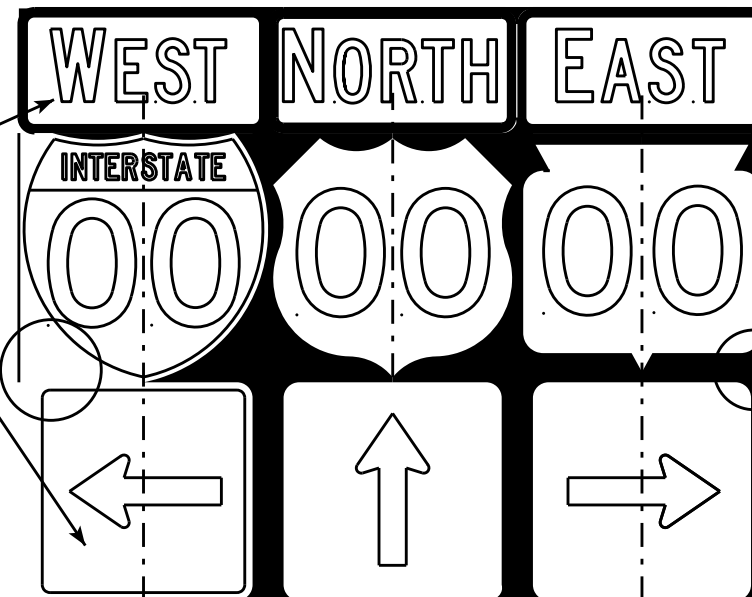
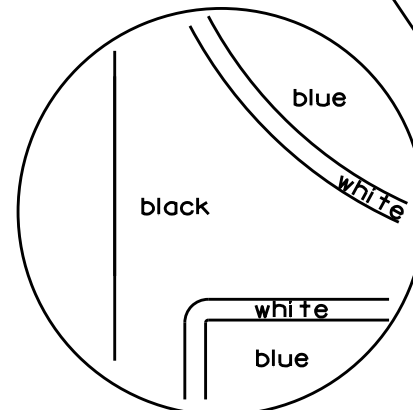
(Typical Vertical J-Assembly
See Note 10 and 11)



JH-1

Blue Background

[blue background
with interstate]



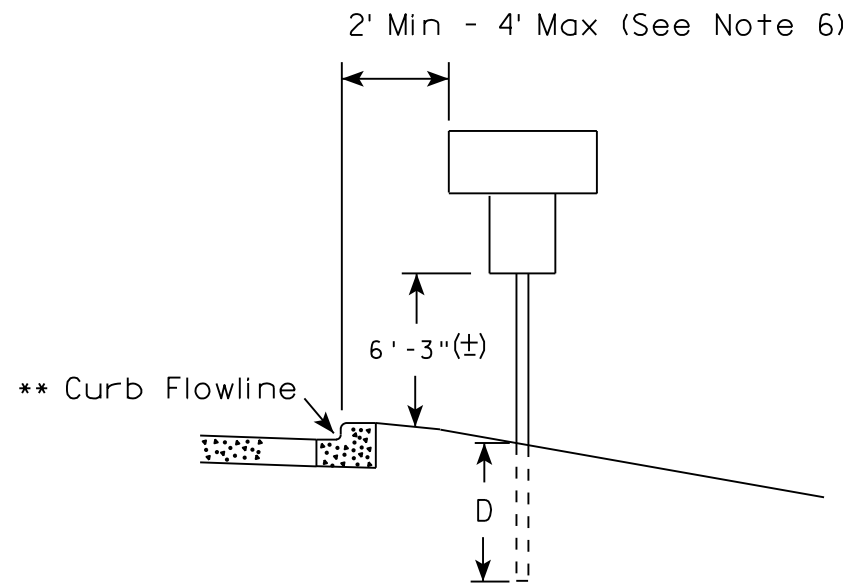
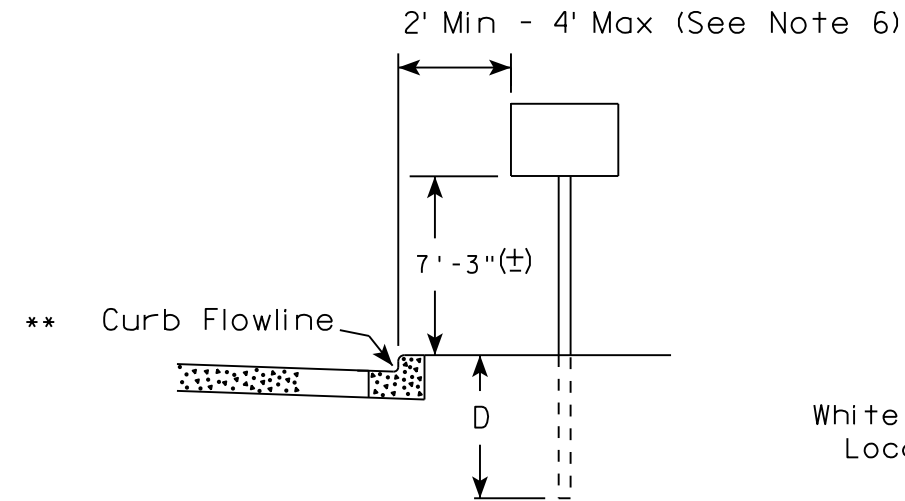
[black background]

ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 2/06/14	PLATE NO. A2-1S.8

NOTES

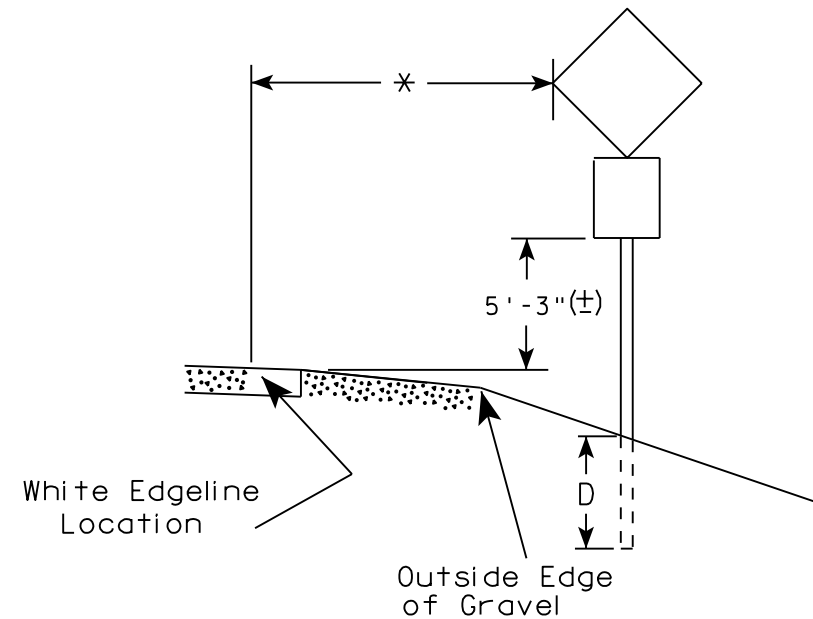
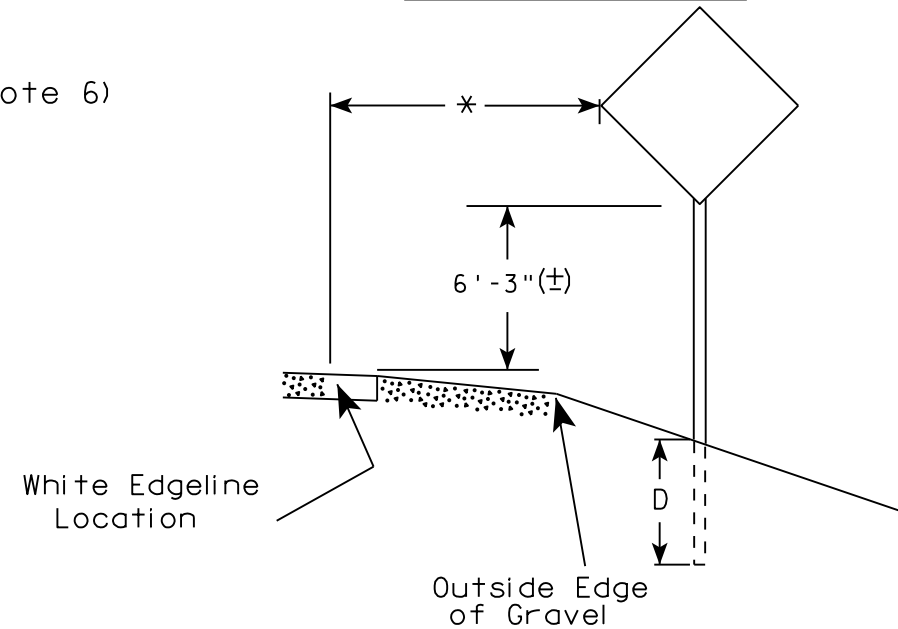
- Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Black Non-reflective
Message - see Note 5
- Message Series - See Note 5
- Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

URBAN AREA



✱✱ The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

RURAL AREA (See Note 2)



* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

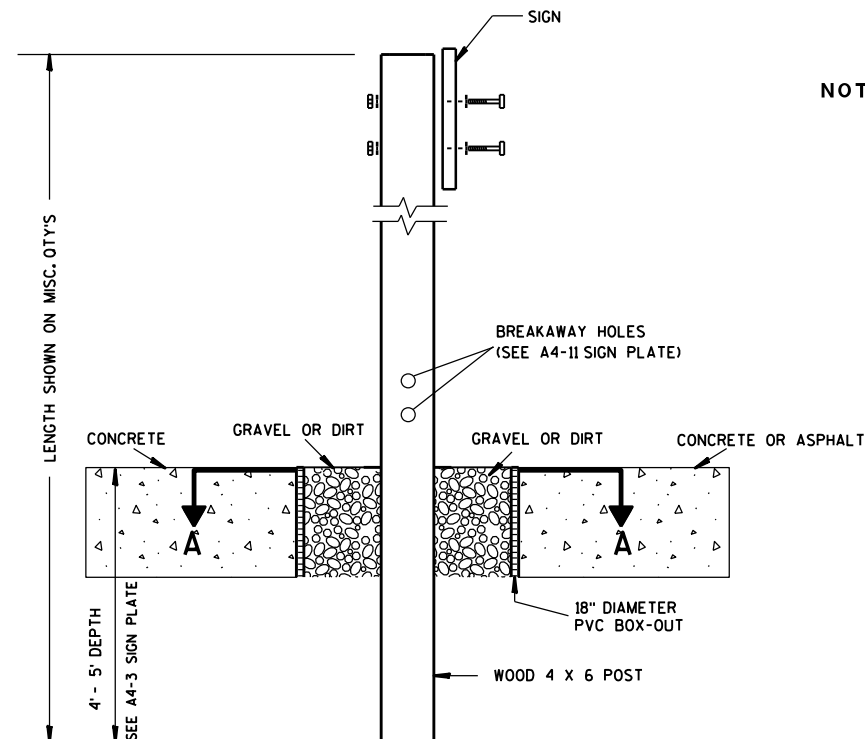
1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. J-Assemblies are considered to be one sign for mounting height.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'-3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

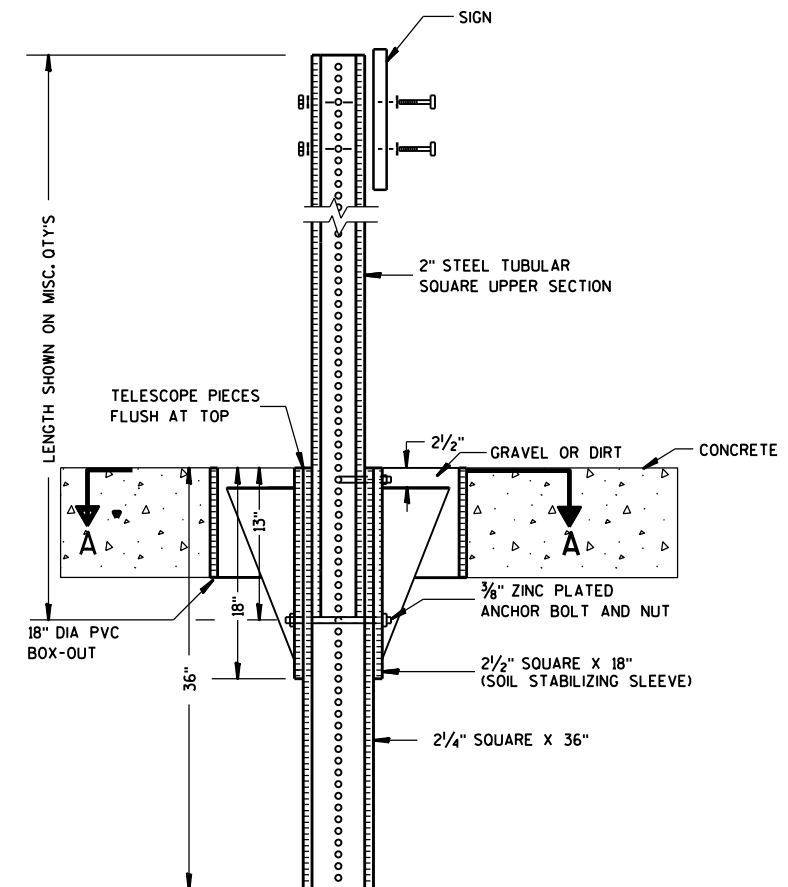
DATE 8/21/17 PLATE NO. A4-3.21



ELEVATION VIEW

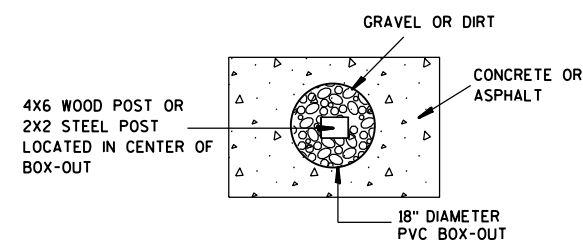
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

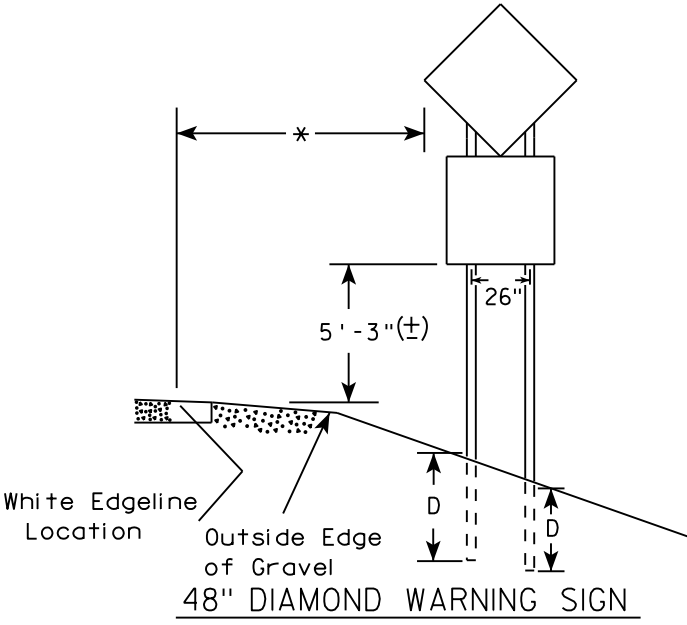
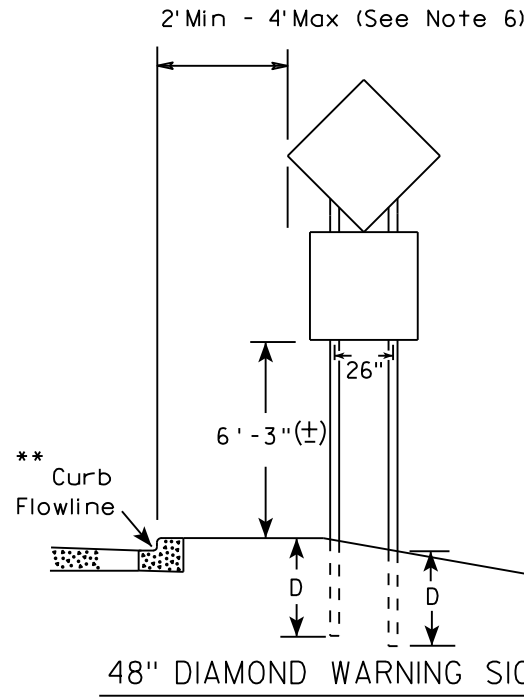
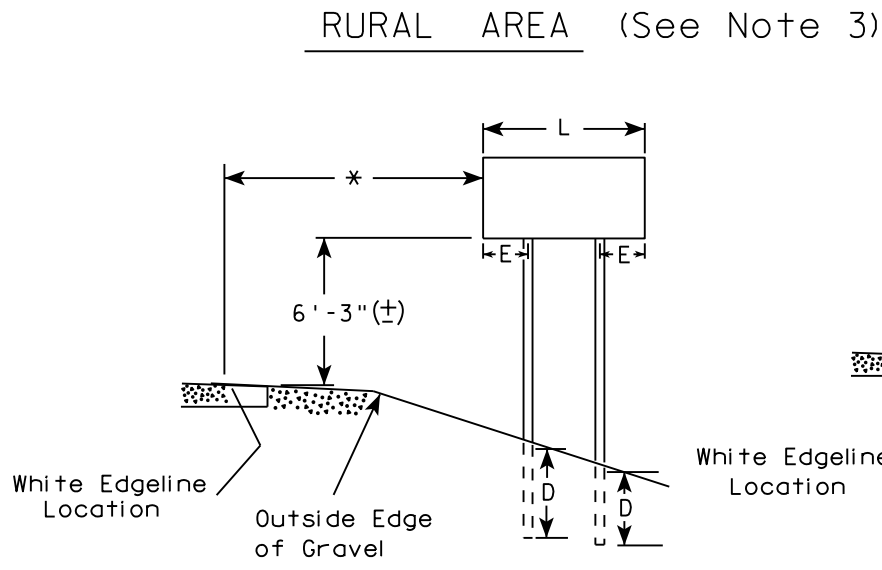
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



- GENERAL NOTES
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. J-Assemblies are considered to be one sign for mounting height.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 8/21/17	PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

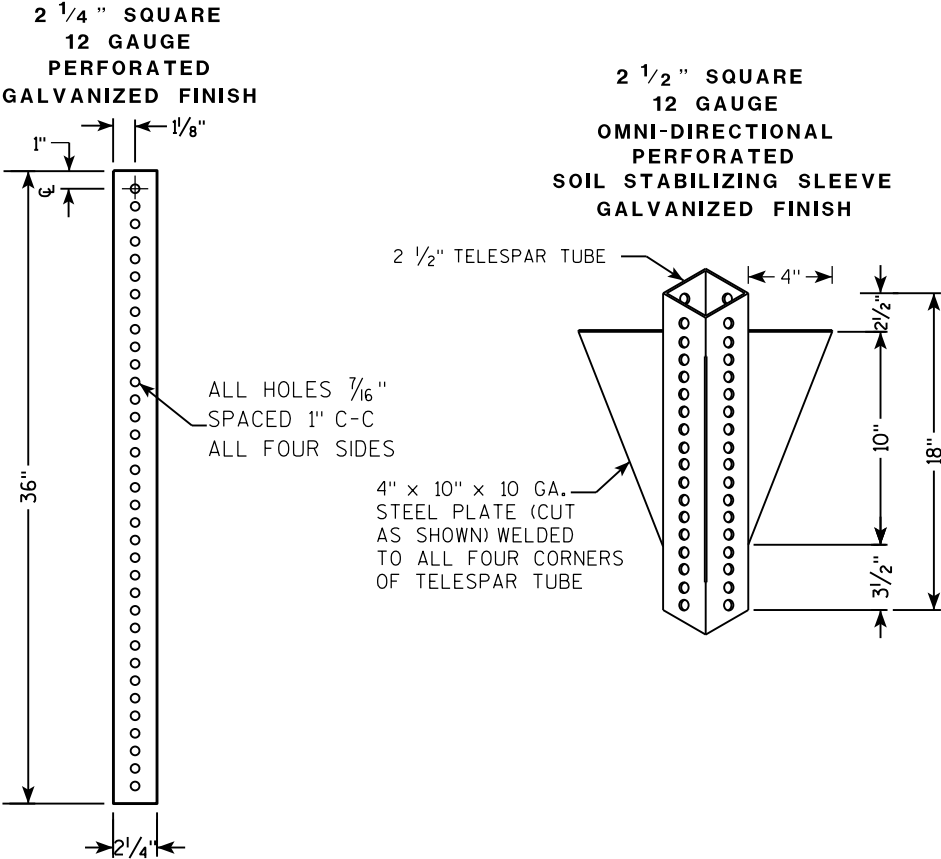
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
 - 3/8" X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - 3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - 9/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 - 1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 8/11/16	PLATE NO. A4-8.8

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM



DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

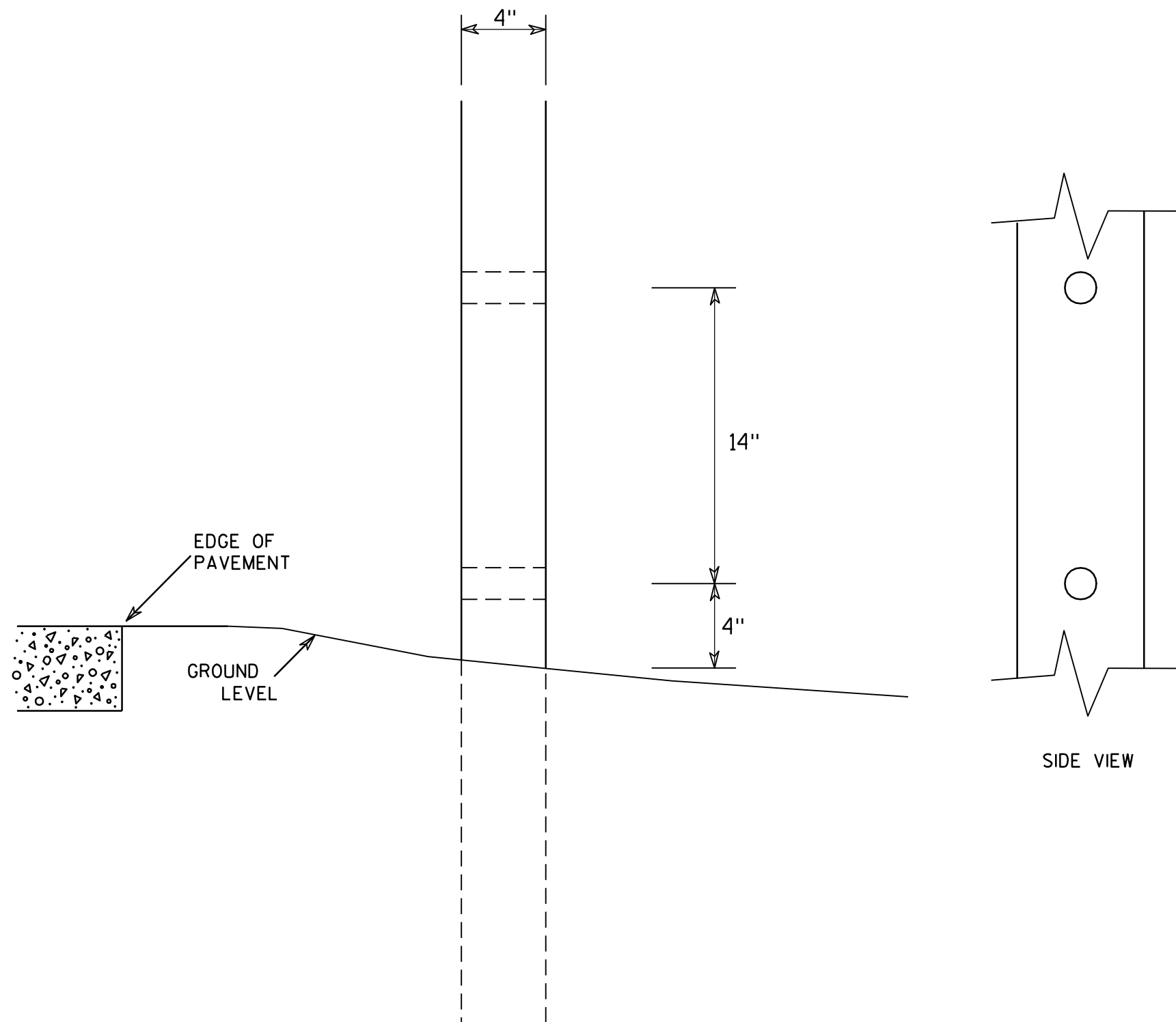
TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

7

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

**4 X 6 WOOD POST
MODIFICATIONS**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

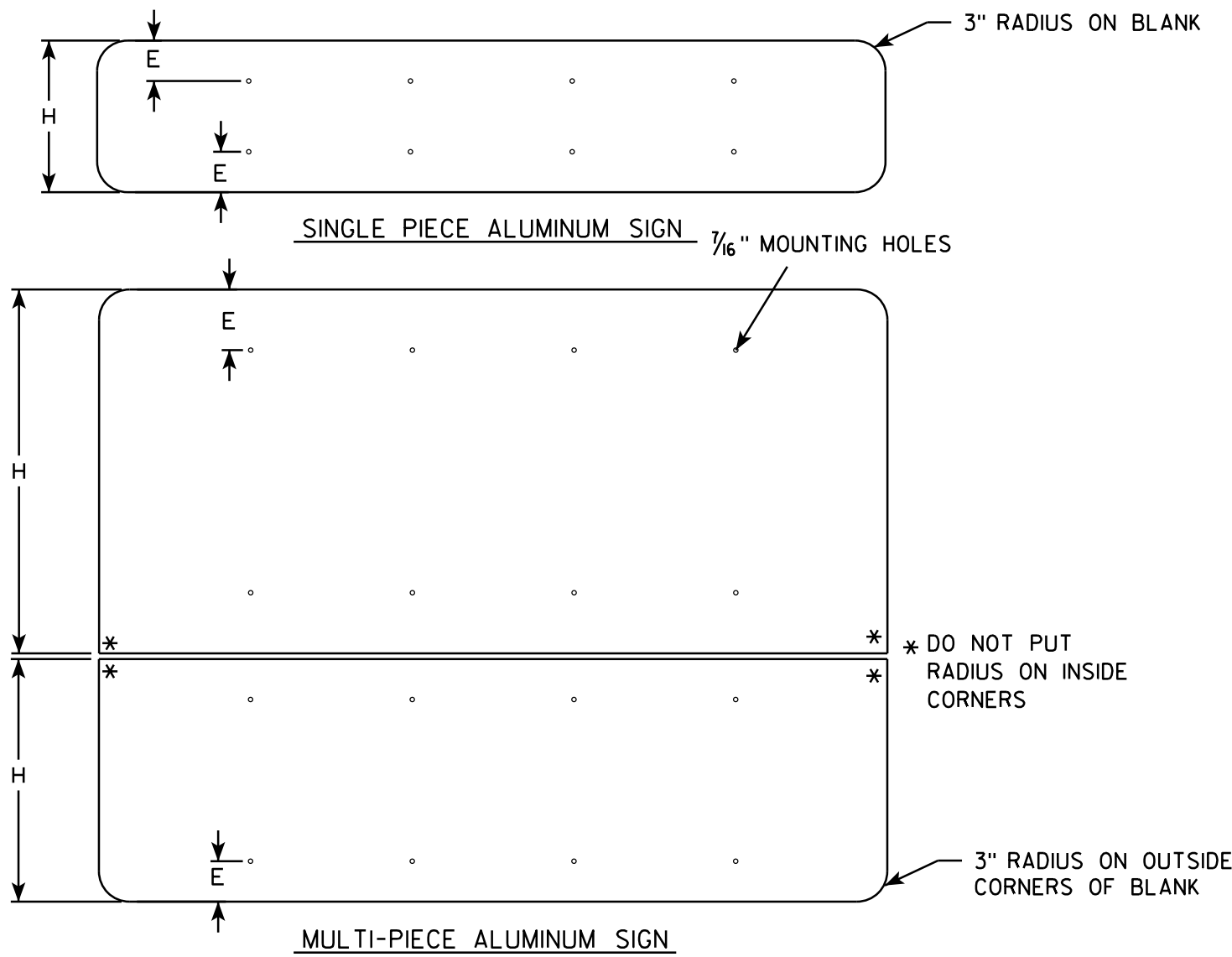
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

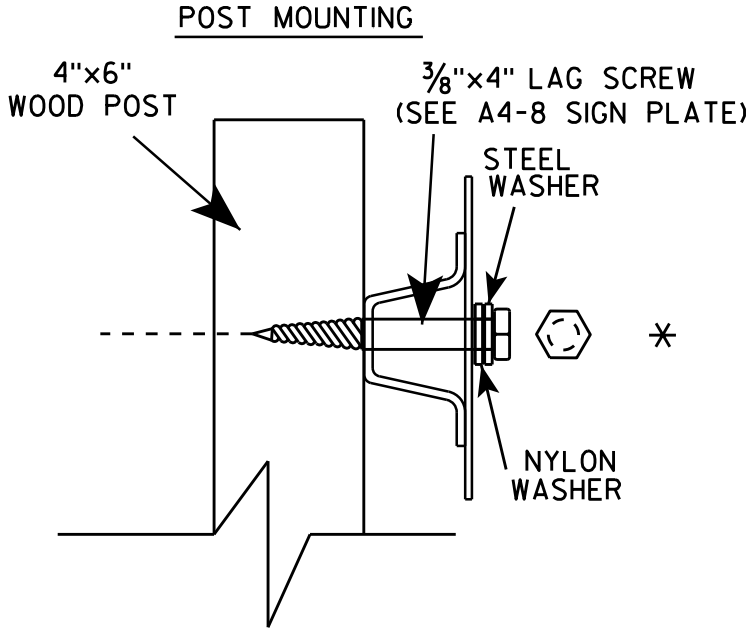
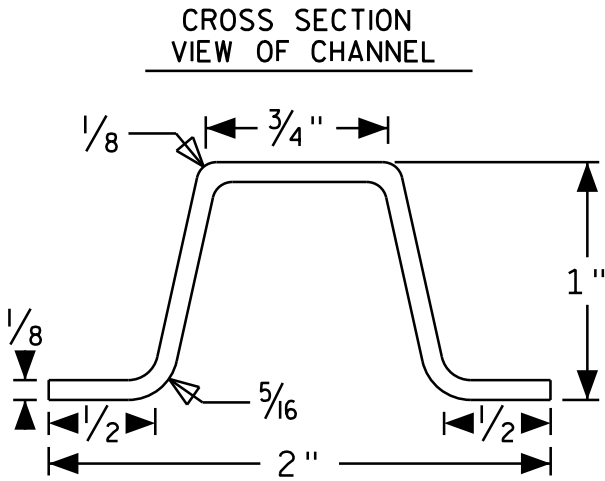
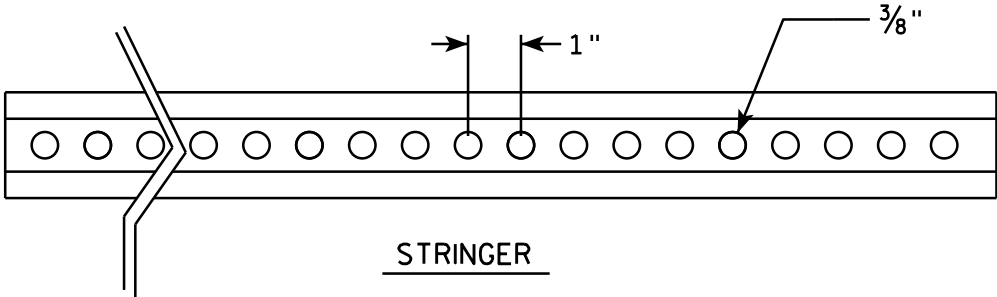
E



GENERAL NOTES

- ALL SIGNS OVER 60" IN WIDTH SHALL HAVE A 3" RADIUS ON THE OUTSIDE CORNERS OF THE ALUMINUM BLANK.
- MOUNTING HOLES SHALL BE 7/16" DIAMETER.
- SEE CHART FOR HOLE SPACING REQUIREMENTS
- FOR SIGN PANELS WITH DIMENSION (H) 36" AND OVER, DIMENSION E SHALL BE 6"
- FOR SIGN PANELS WITH DIMENSION (H) UNDER 36", DIMENSION E SHALL BE 4"
- SIGN STRINGER MATERIAL SHALL CONSIST OF STEEL CHANNEL POST SECTIONS, WEIGHING 1.12 LBS/FT IN ACCORDANCE WITH SECTION 633.2.1 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- SEE SIGN PLATE A4-8 FOR SIGN STRINGER BOLTING REQUIREMENTS.

SIGN WIDTH	STRINGER WIDTH	POSTS	HOLE SPACING	MOUNTING HOLES			
78"	72"	2	16"	15"	31"	47"	63"
84"	72"	2	17"	16 1/2"	33 1/2"	50 1/2"	67 1/2"
90"	72"	2	18"	18"	36"	54"	72"
96"	90"	2	19"	19 1/2"	38 1/2"	57 1/2"	76 1/2"
102"	90"	2	20"	21"	41"	61"	81"
108"	90"	2	21"	22 1/2"	43 1/2"	64 1/2"	85 1/2"
114"	108"	3	15"	12"	27"	42"	57" 72" 87" 102"
120"	108"	3	16"	12"	28"	44"	60" 76" 92" 108"
126"	108"	3	17"	12"	29"	46"	63" 80" 97" 114"
132"	126"	3	18"	12"	30"	48"	66" 84" 102" 120"
138"	126"	3	19"	12"	31"	50"	69" 88" 107" 126"
144"	126"	3	20"	12"	32"	52"	72" 92" 112" 132"



SIGN STRINGER
MOUNTING REQUIREMENTS

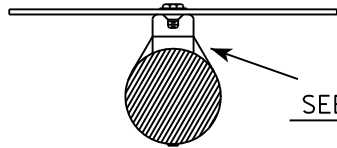
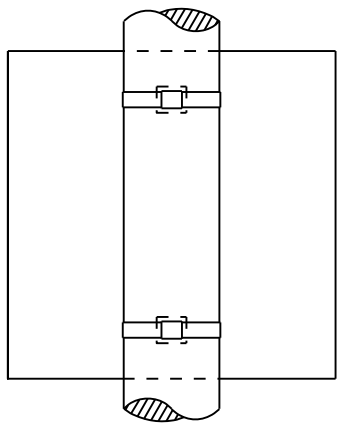
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

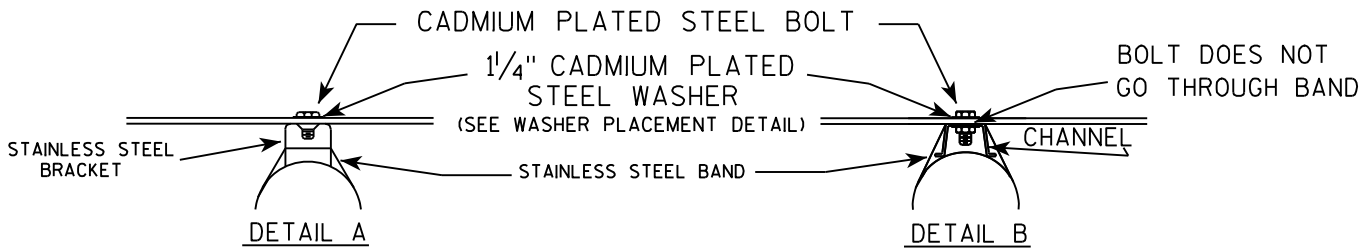
DATE 4/26/16 PLATE NO. A4-18.1

BANDING

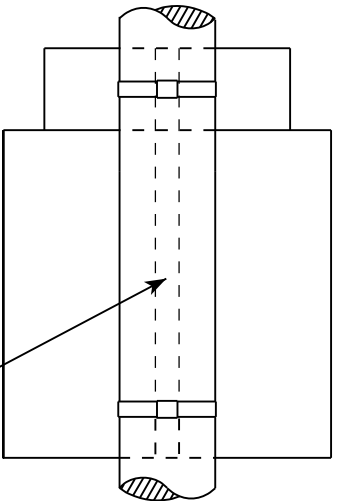
SINGLE SIGN



SEE DETAIL A



"J" ASSEMBLY



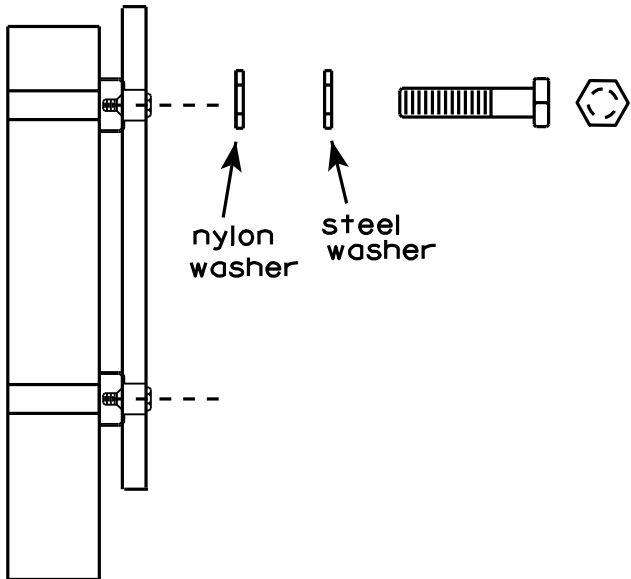
CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET

SEE DETAIL B

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.

WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

DATE 8/16/13

PLATE NO. A5-9.3

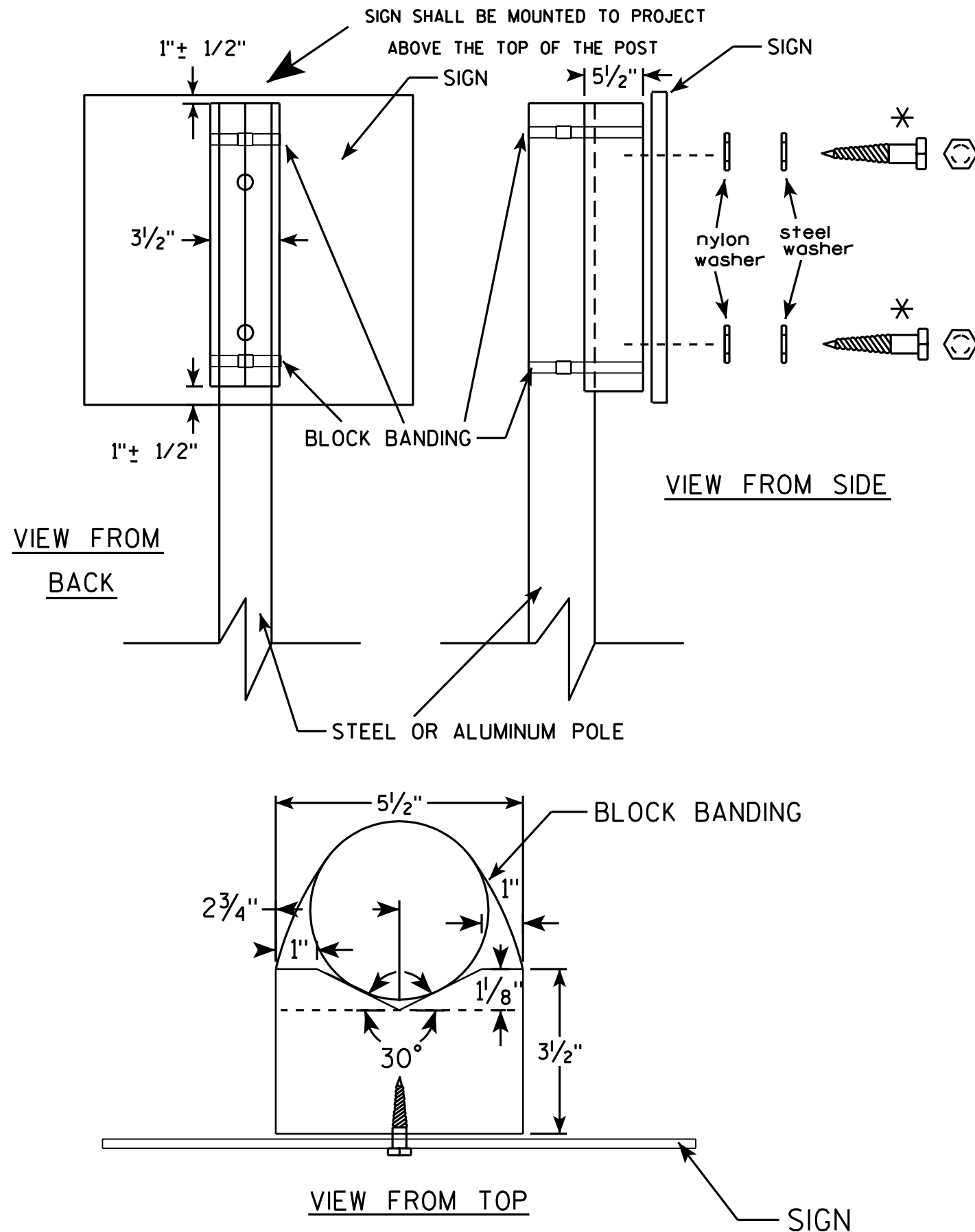
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. WOOD 4"x6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D, or
 - b. Cadmium plated in accordance with ASTM Designation : B 766 TYPE 3, Class 12, or
 - c. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

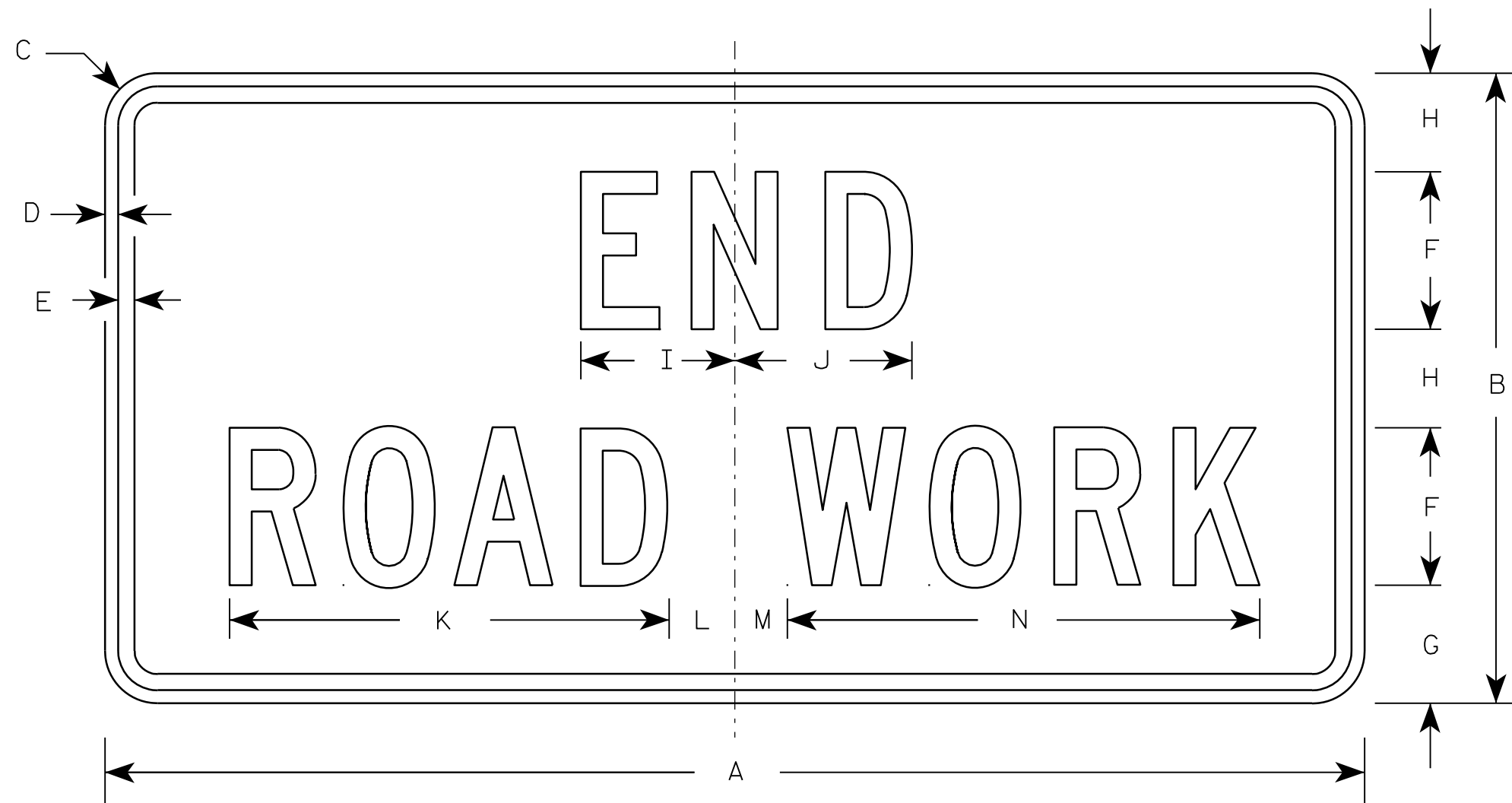
DATE 7/12/07 PLATE NO. A5-10.1

PROJECT NO:

SHEET NO:

E

7



G20-2A

Metric equivalent
for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

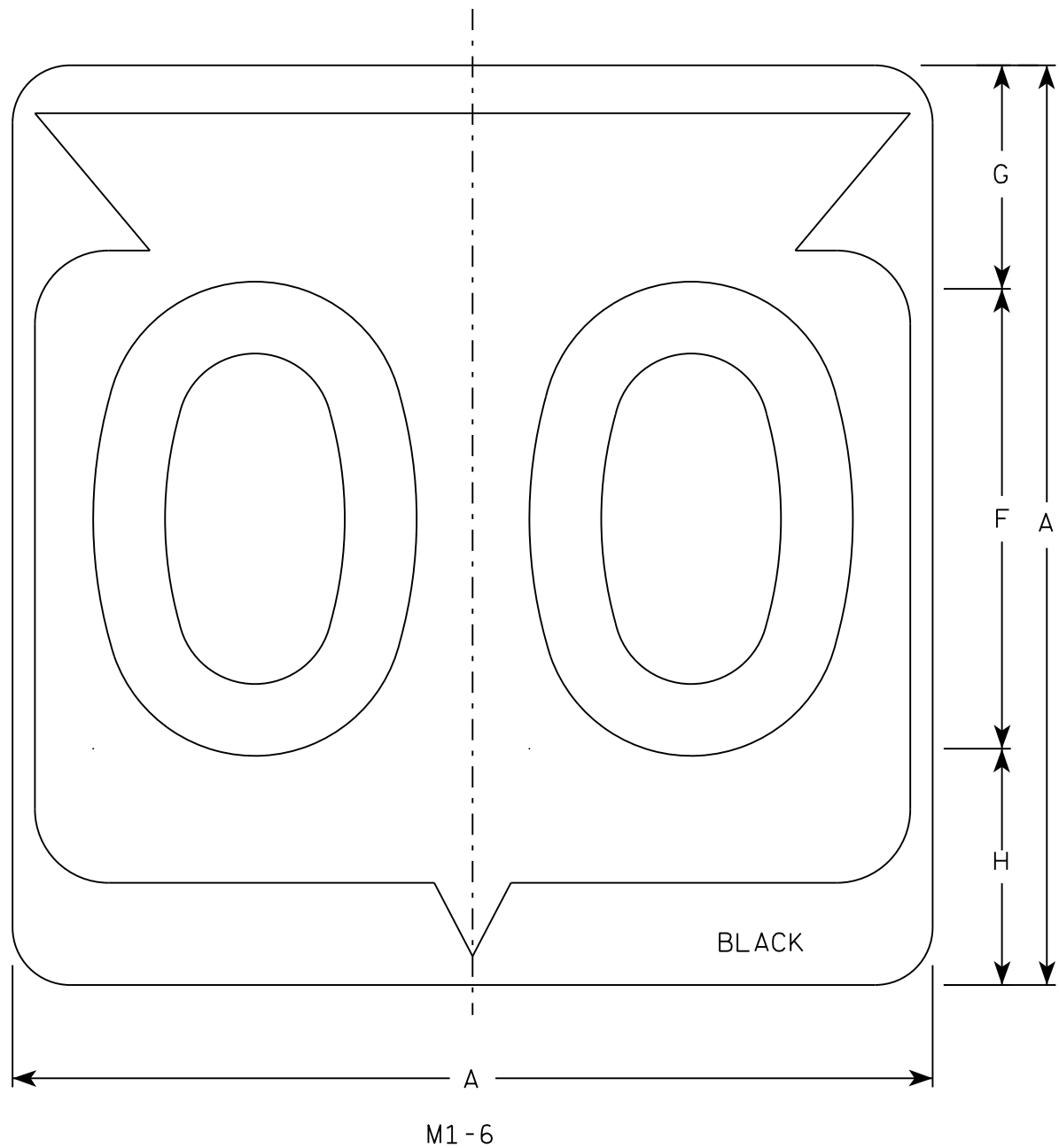
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area sq. m.
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

NOTES

- Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Orange
Message - Black
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

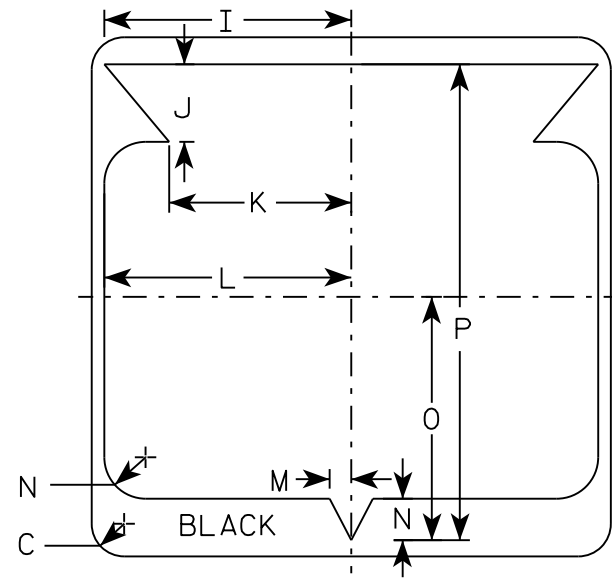
7

7



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

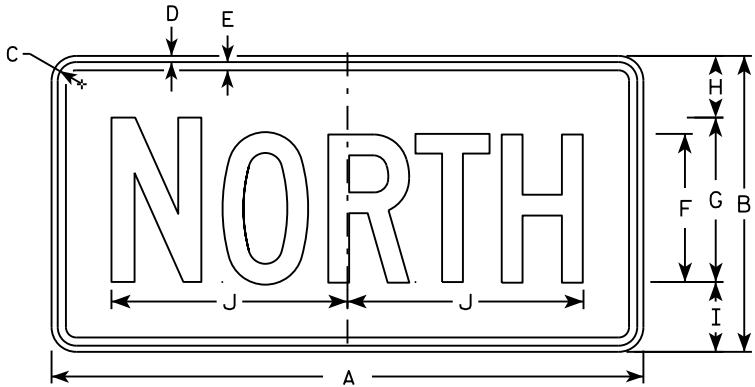
PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

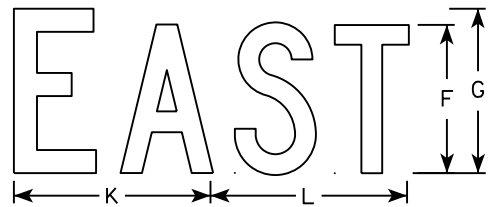
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

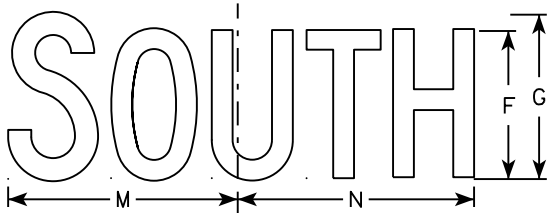
DATE 3/16/18 PLATE NO. M1-6.10



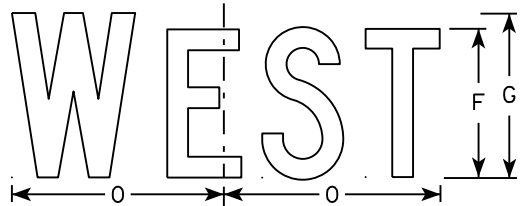
M3-1
MM3-1
MP3-1



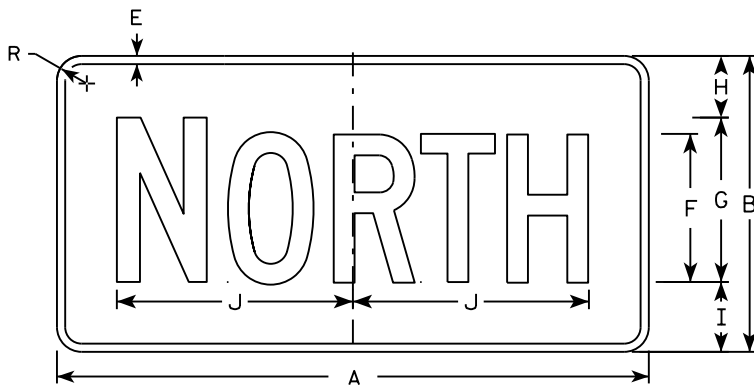
M3-2
MM3-2
MP3-2



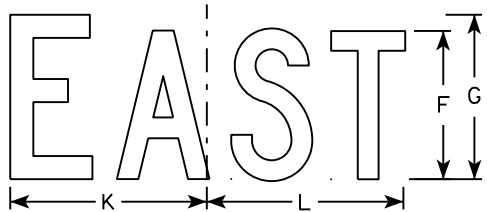
M3-3
MM3-3
MP3-3



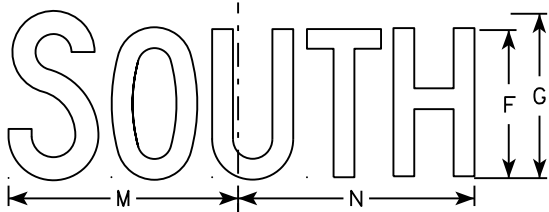
M3-4
MM3-4
MP3-4



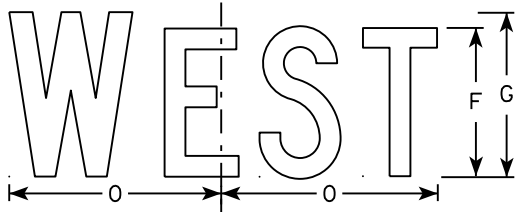
MB3-1
MK3-1
MN3-1



MB3-2
MK3-2
MN3-2



MB3-3
MK3-3
MN3-3



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

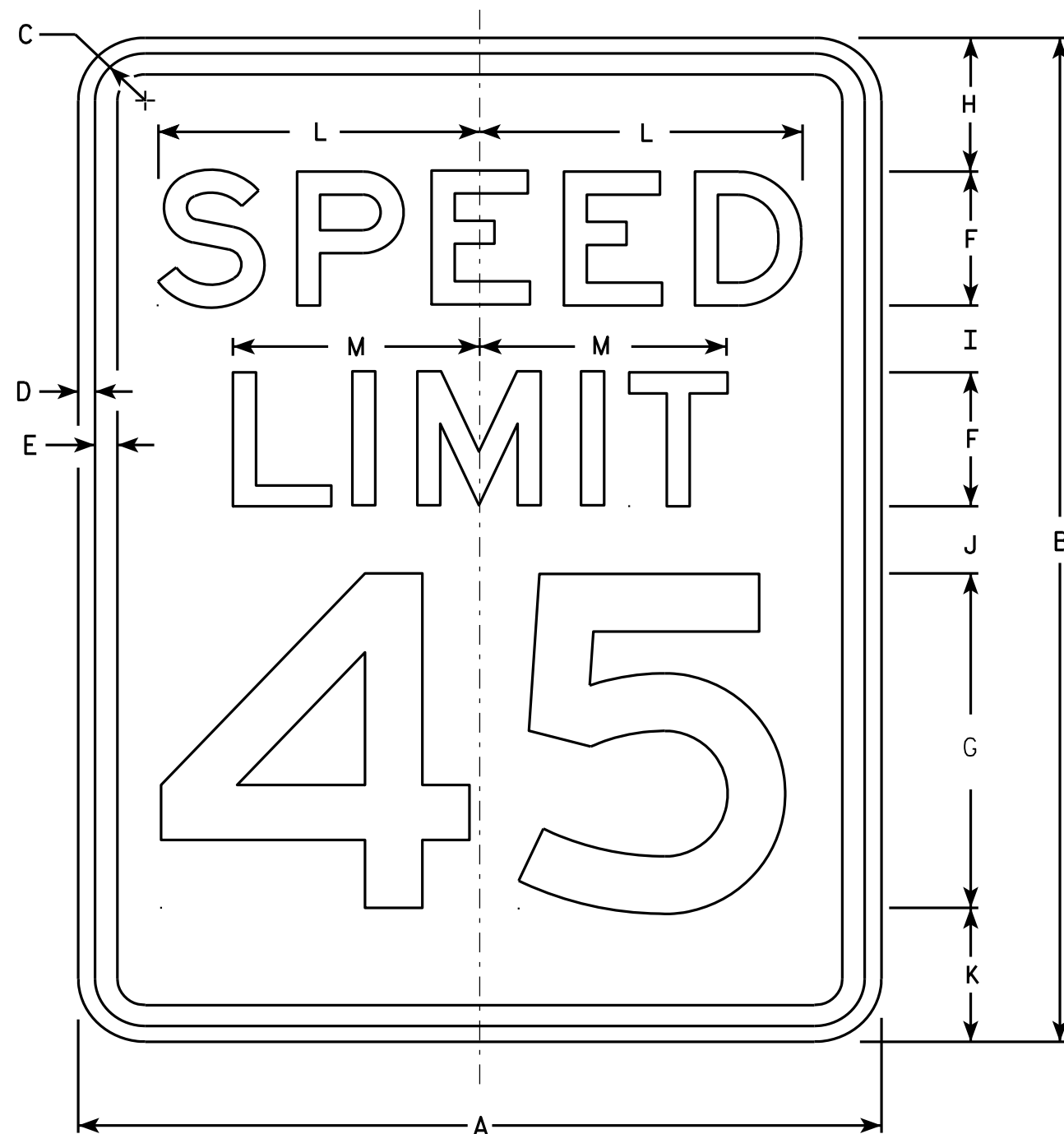
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

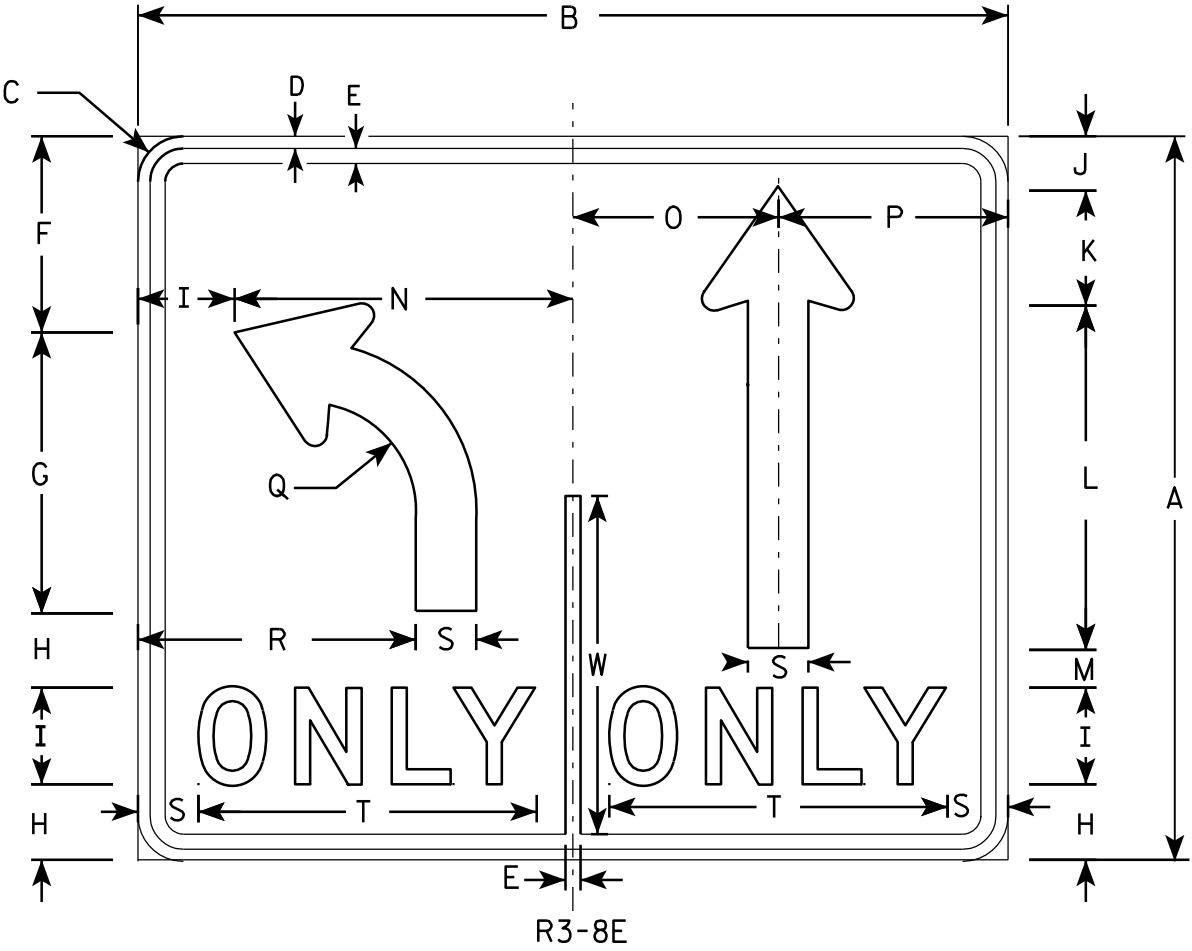
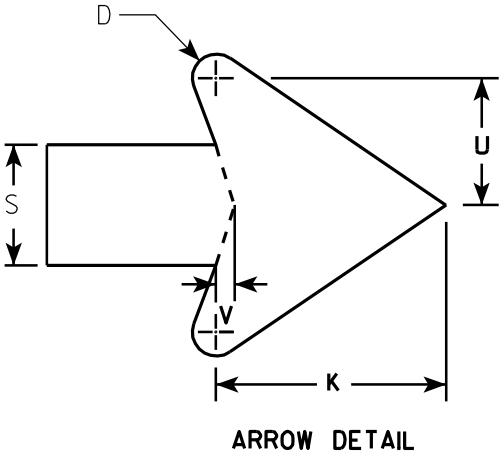
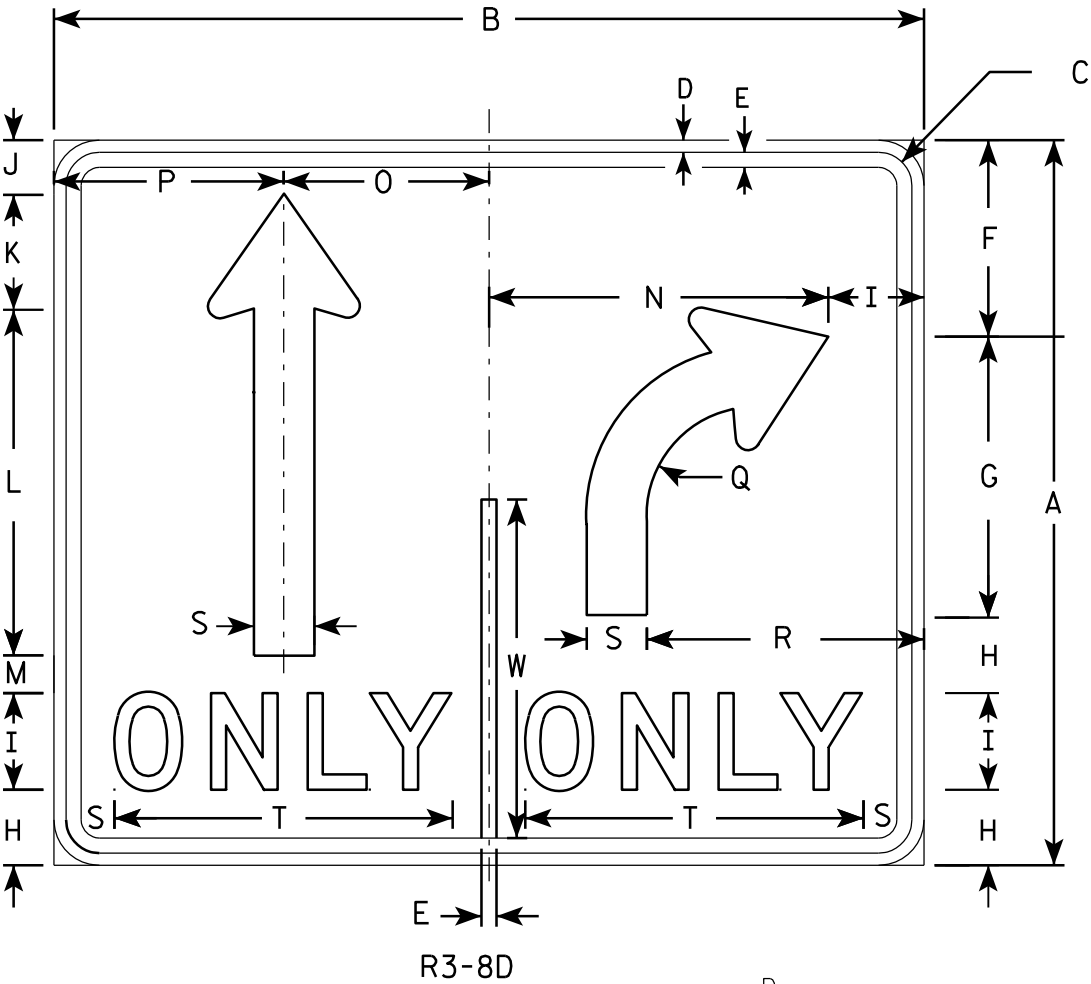
STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: HWY: COUNTY: SHEET NO: E

NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - WHITE
Message - BLACK
- 3. Message Series - D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



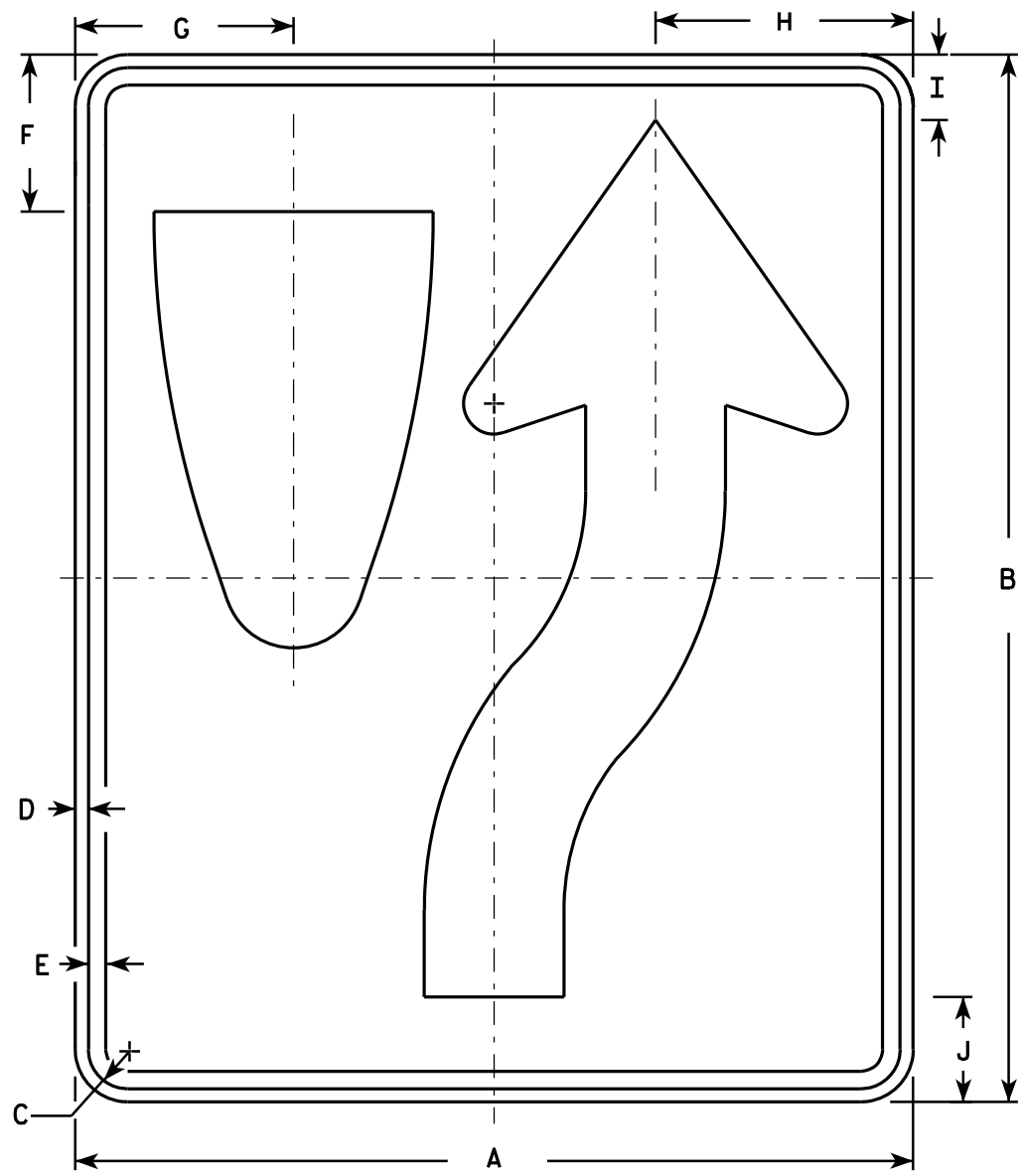
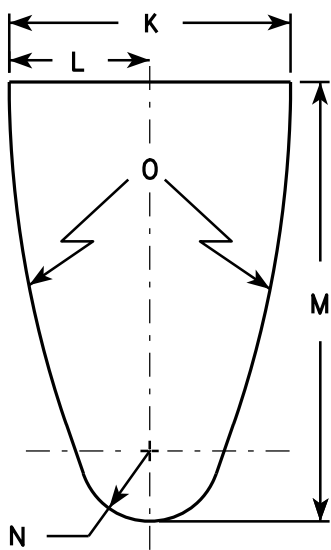
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3⁄8	1⁄2	5⁄8	8 1⁄8	11 5⁄8	3 1⁄8	4	2 1⁄4	4 3⁄4	14 1⁄4	1 5⁄8	14	8 1⁄2	9 1⁄2	4 1⁄2	11 1⁄2	2 1⁄2	14	2 5⁄8	3⁄8	14				7.5
2M	30	36	1 3⁄8	1⁄2	5⁄8	8 1⁄8	11 5⁄8	3 1⁄8	4	2 1⁄4	4 3⁄4	14 1⁄4	1 5⁄8	14	8 1⁄2	9 1⁄2	4 1⁄2	11 1⁄2	2 1⁄2	14	2 5⁄8	3⁄8	14				7.5
3																											
4	48	54	2 1⁄4	3⁄4	1	13 1⁄4	18 1⁄2	5 1⁄8	6	3 1⁄2	7 1⁄8	21 1⁄2	4 3⁄4	21	12 3⁄4	14 1⁄4	7 1⁄4	17 1⁄8	3 3⁄4	20 5⁄8	4	5⁄8	22 3⁄8				18.0
5	48	54	2 1⁄4	3⁄4	1	13 1⁄4	18 1⁄2	5 1⁄8	6	3 1⁄2	7 1⁄8	21 1⁄2	4 3⁄4	21	12 3⁄4	14 1⁄4	7 1⁄4	17 1⁄8	3 3⁄4	20 5⁄8	4	5⁄8	22 3⁄8				18.0

STANDARD SIGN
R3-8D & R3-8E

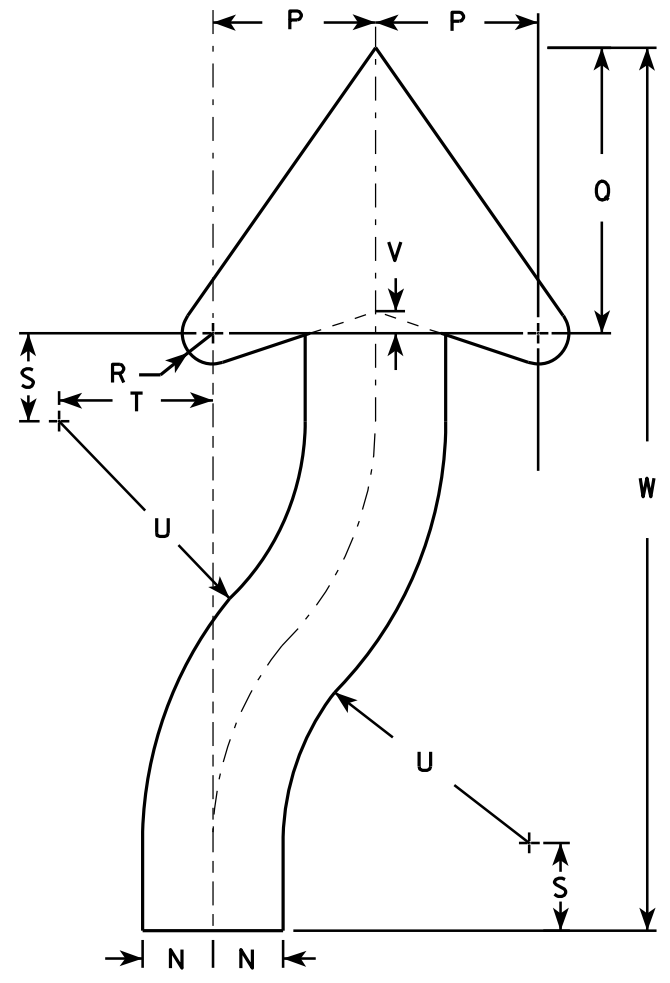
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 3/18/2011 PLATE NO. R3-8D.2

NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
- 2. Color:
Background - White
Message - Black
- 3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
- 4. R4-8 is the same as R4-7 except Legend is reversed.



R4-7



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3 3/8	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5/8	1 7/8	3 1/4	6 3/4	1/2	20 3/8				3.0
2S	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2M	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 5/8	5	8 3/4	18	1 1/4	50 1/4				20.0

STANDARD SIGN
R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-7.8

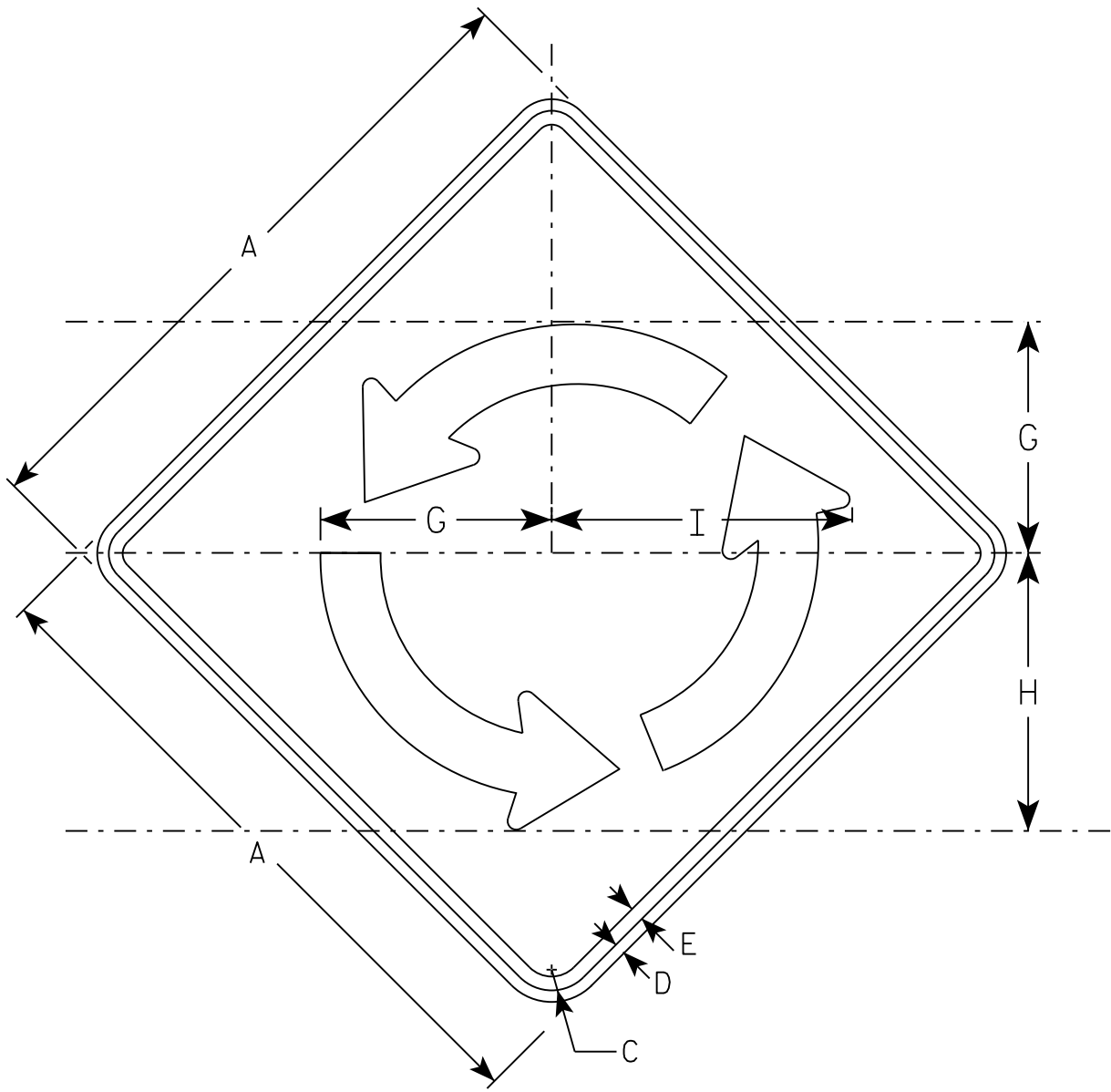
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



W2-6

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - YELLOW
Message - BLACK

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Area sq. ft.
1																									
2S	30		1 3/8	1/2	5/8		10 3/8	12 1/2	13 1/2																6.25
2M	30		1 3/8	1/2	5/8		10 3/8	12 1/2	13 1/2																6.25
3	36		1 5/8	5/8	3/4		12 1/2	15	16 1/4																9.00
4	48		2 1/4	3/4	1		16 5/8	20	16 1/4																16.0
5																									

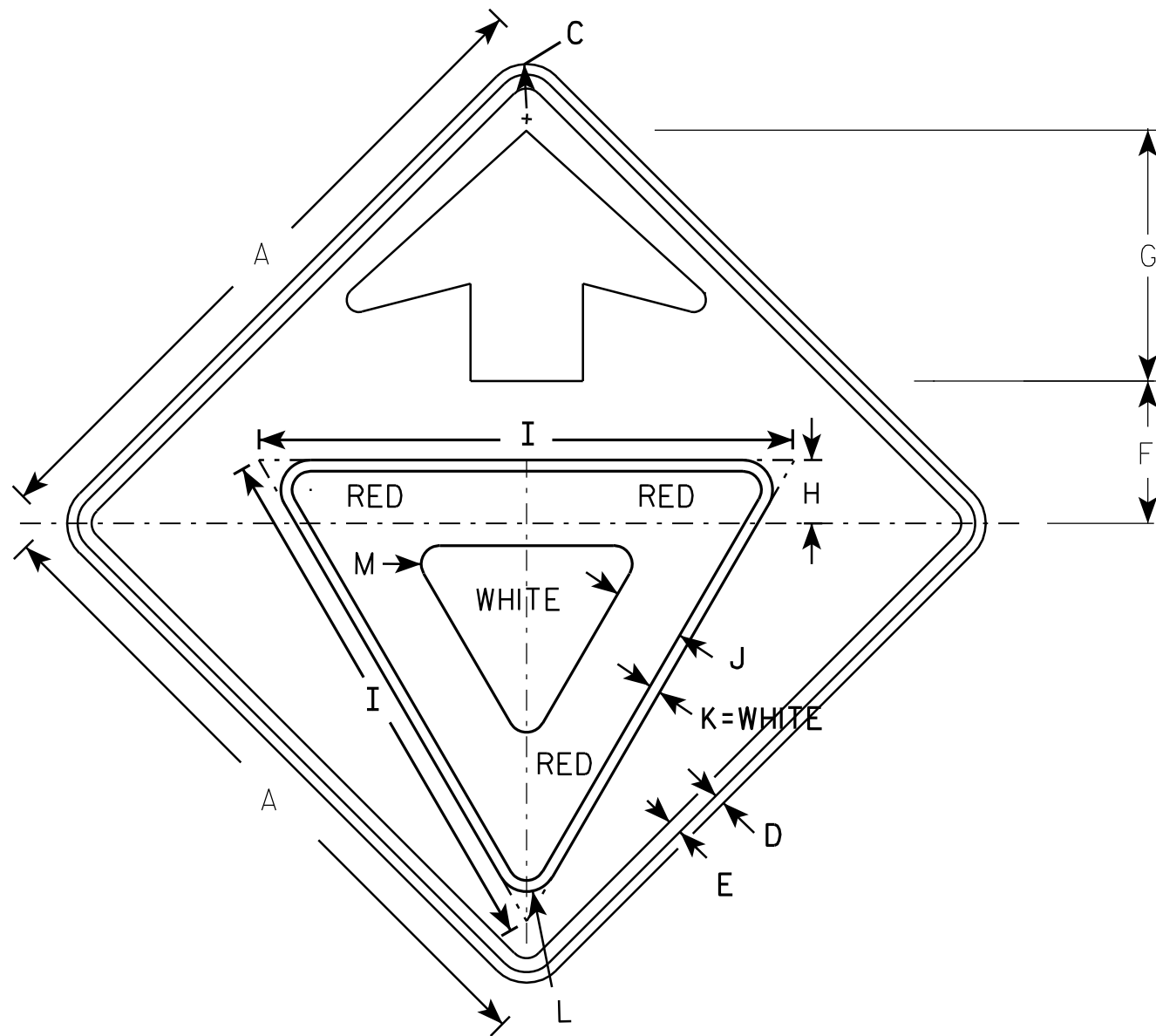
STANDARD SIGN

W2-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

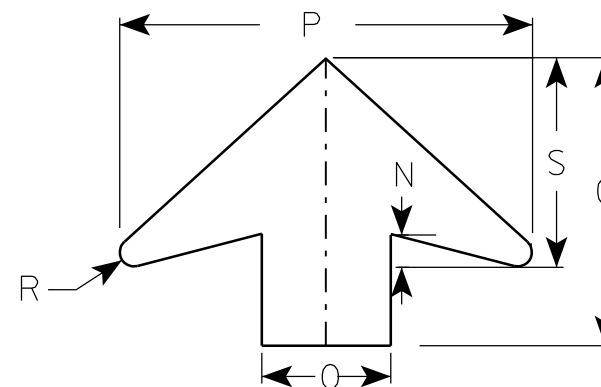
DATE 3/15/18 PLATE NO. W2-6.6



W3-2

NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - YELLOW
Arrow & Border - BLACK
Yield Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	3	25	3 3/8	1/2	1 3/8	7/8	1 1/4	5	16		1/2	8								6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 3/8	28	3 3/4	5/8	1 1/2	1	1 5/8	6	19 1/4		5/8	9 3/4								9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 3/8	28	3 3/4	5/8	1 1/2	1	1 5/8	6	19 1/4		5/8	9 3/4								9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 3/8	28	3 3/4	5/8	1 1/2	1	1 5/8	6	19 1/4		5/8	9 3/4								9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	38	5	3/4	2 1/8	1 3/8	2	8	25 5/8		7/8	13								16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	38	5	3/4	2 1/8	1 3/8	2	8	25 5/8		7/8	13								16.0

PROJECT NO:

STANDARD SIGN
W3-2

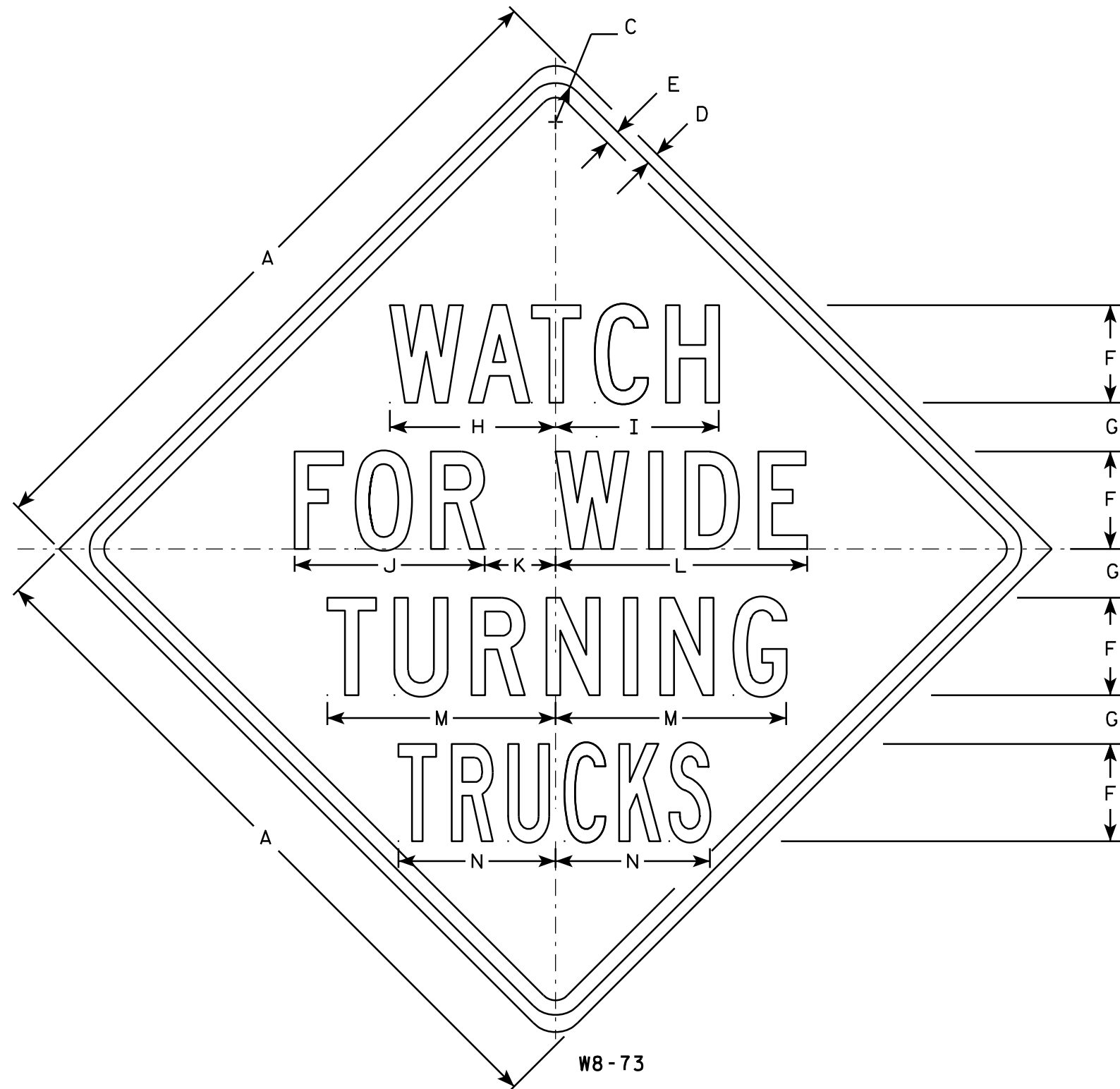
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W3-2..9

SHEET NO:

E



NOTES

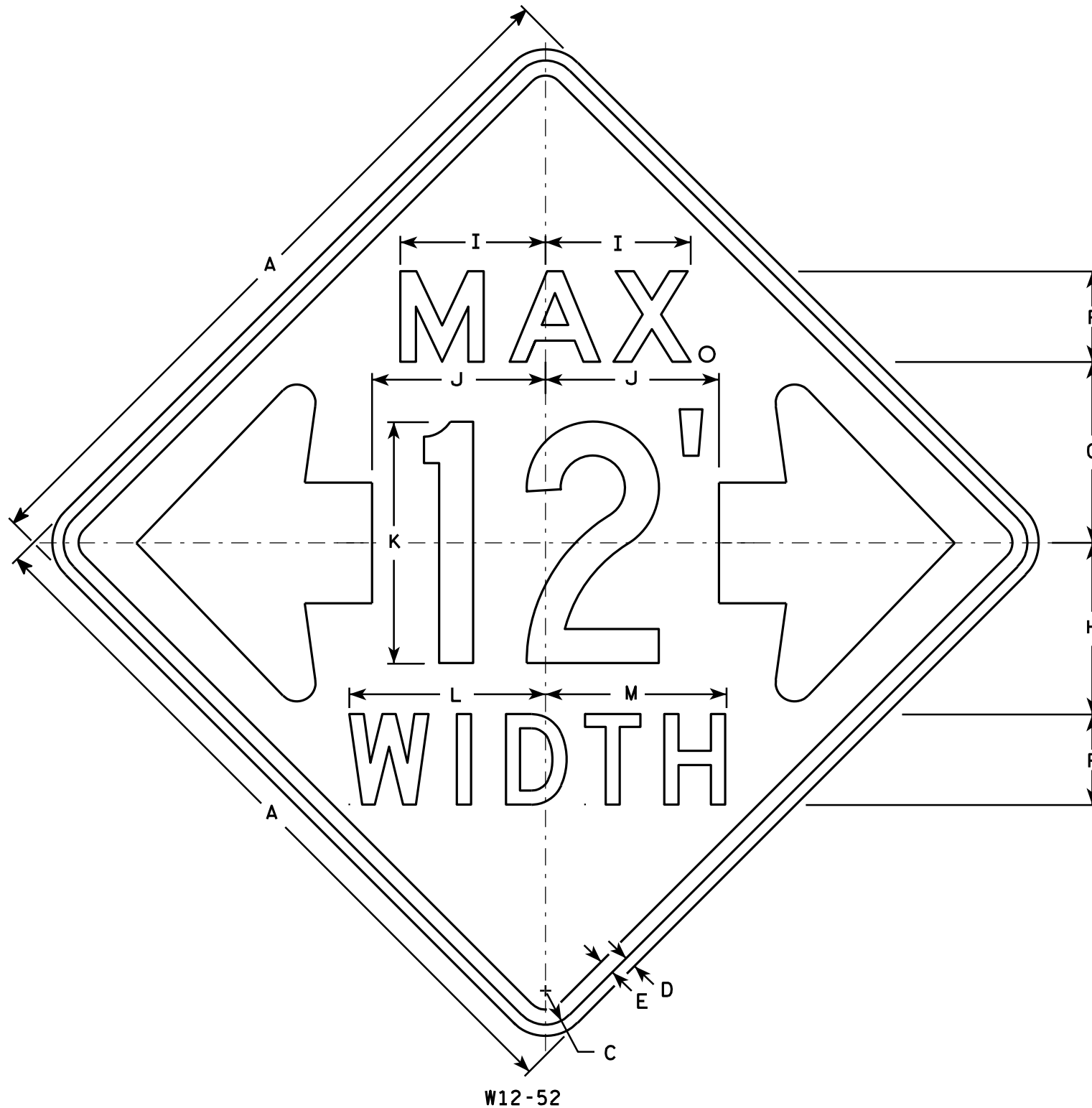
1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - C except line 4 Series B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 3⁄8	1⁄2	5⁄8	4	2	6 7⁄8	6 3⁄4	7 7⁄8	3	10 3⁄8	9 1⁄2	6 1⁄2													6.25
2M	36		1 5⁄8	5⁄8	3⁄4	5	2 1⁄2	8 1⁄2	8 3⁄8	9 3⁄4	3 5⁄8	12 7⁄8	11 3⁄4	8													9.0
3	36		1 5⁄8	5⁄8	3⁄4	5	2 1⁄2	8 1⁄2	8 3⁄8	9 3⁄4	3 5⁄8	12 7⁄8	11 3⁄4	8													9.0
4																											
5																											

STANDARD SIGN W8-73

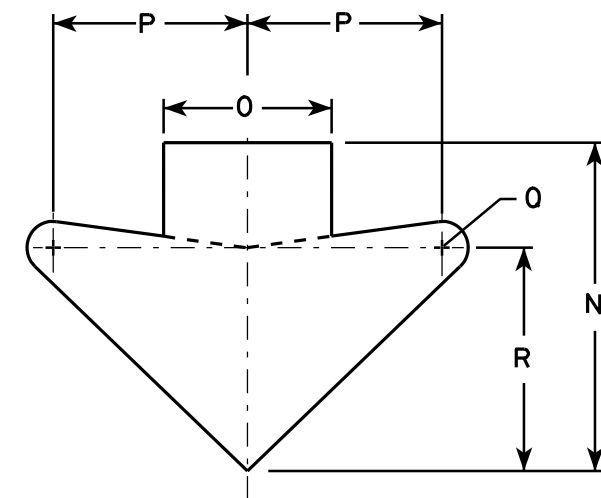
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 03/18/13 PLATE NO. W8-73.3

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

- Sign Is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Orange
Message - Black
- Message Series - See note 5
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- The top line is series E, the numerals are series C, and the bottom line is series D.
- Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8									16.0
2M	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8									16.0
3																											
4																											
5																											

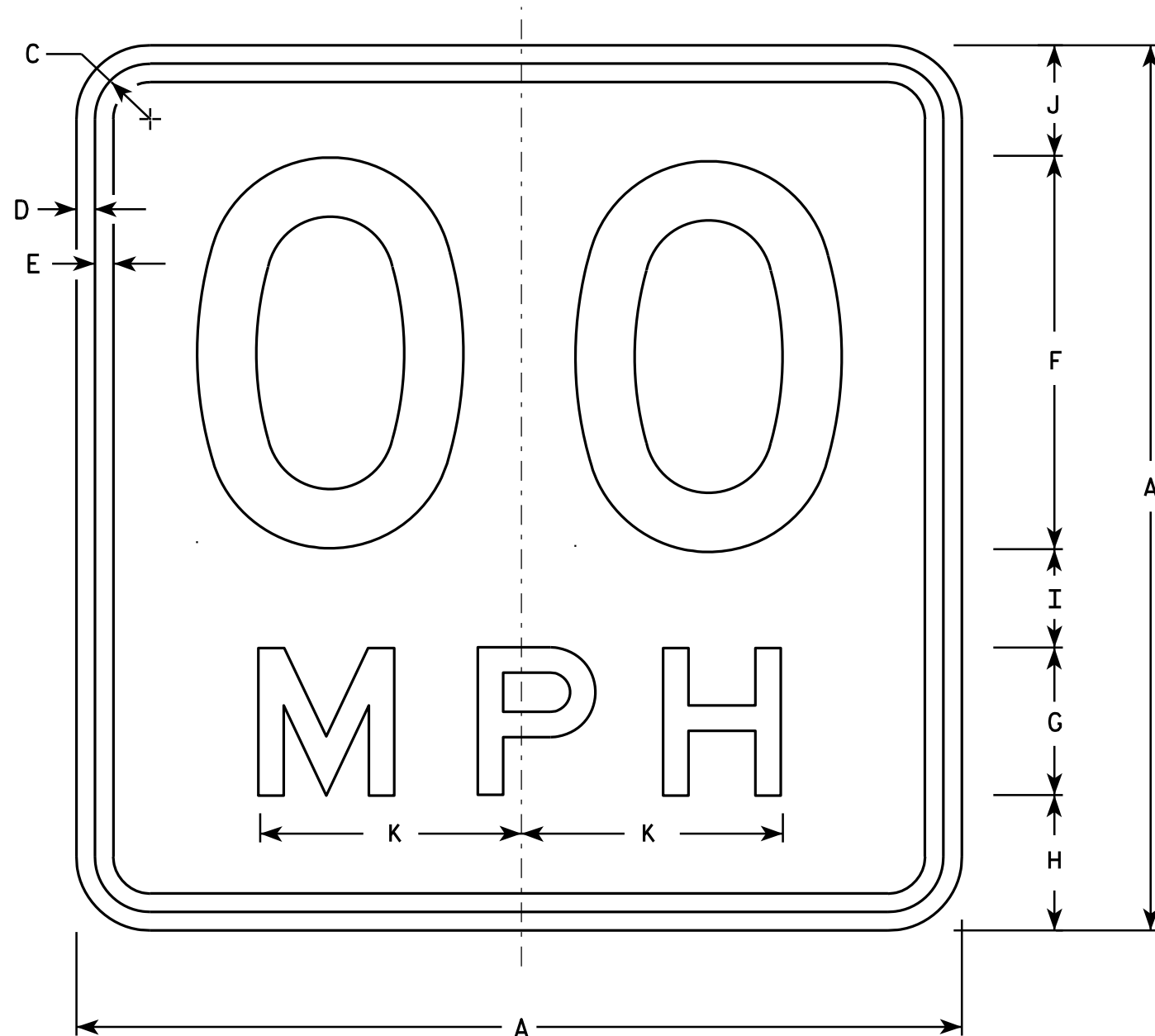
STANDARD SIGN W12-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/16/11 PLATE NO. W12-52.7

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

W13-1

- * For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area Sq. Ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

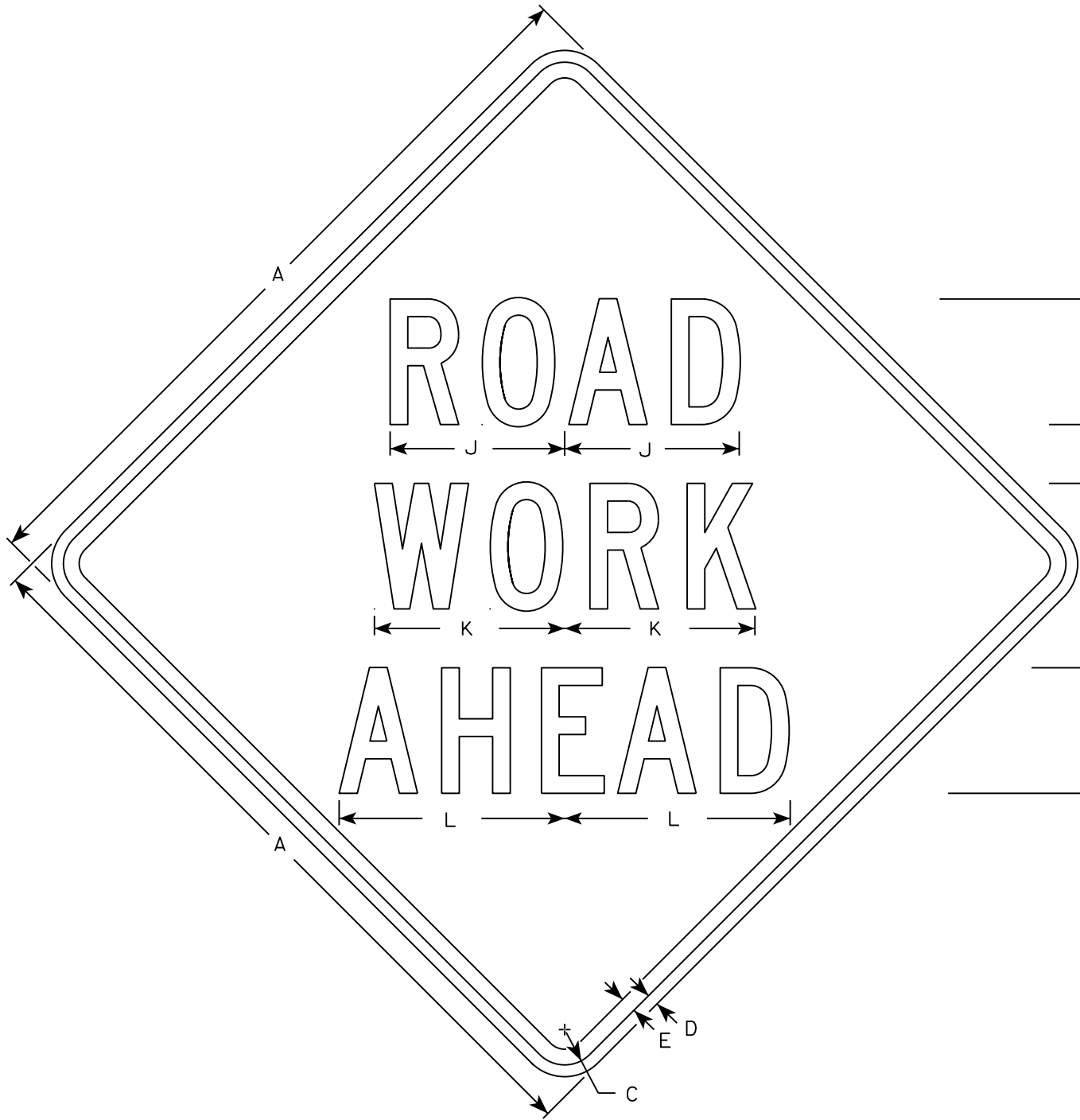
PROJECT NO:

HWY:

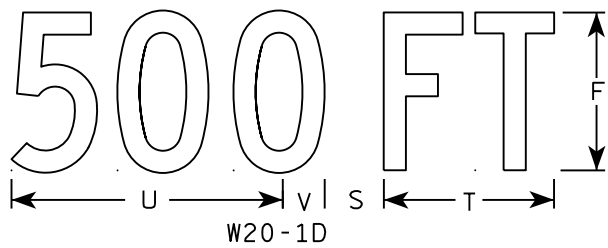
COUNTY:

SHEET NO:

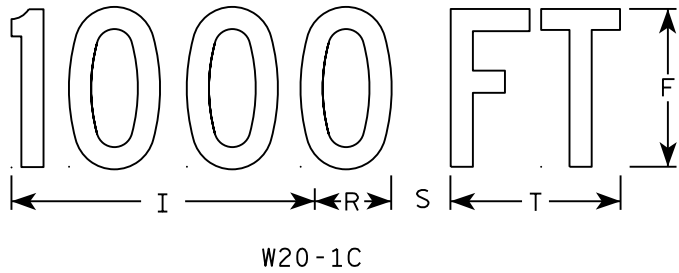
E



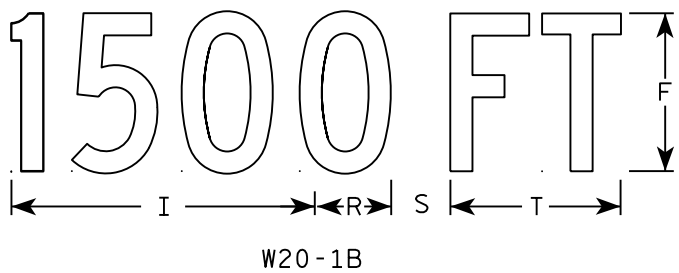
W20-1A



W20-1D



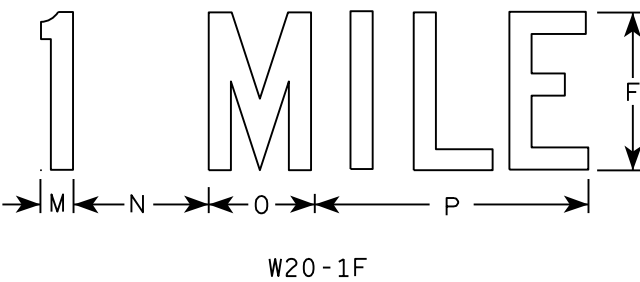
W20-1C



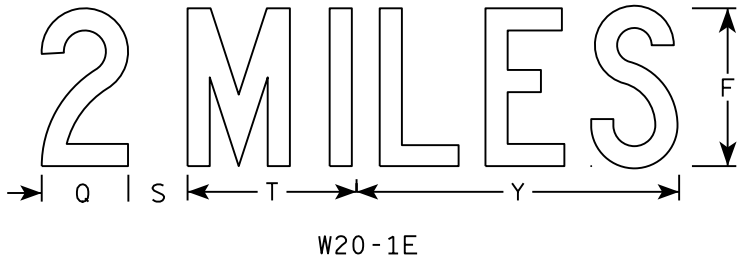
W20-1B



W20-1G



W20-1F



W20-1E

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

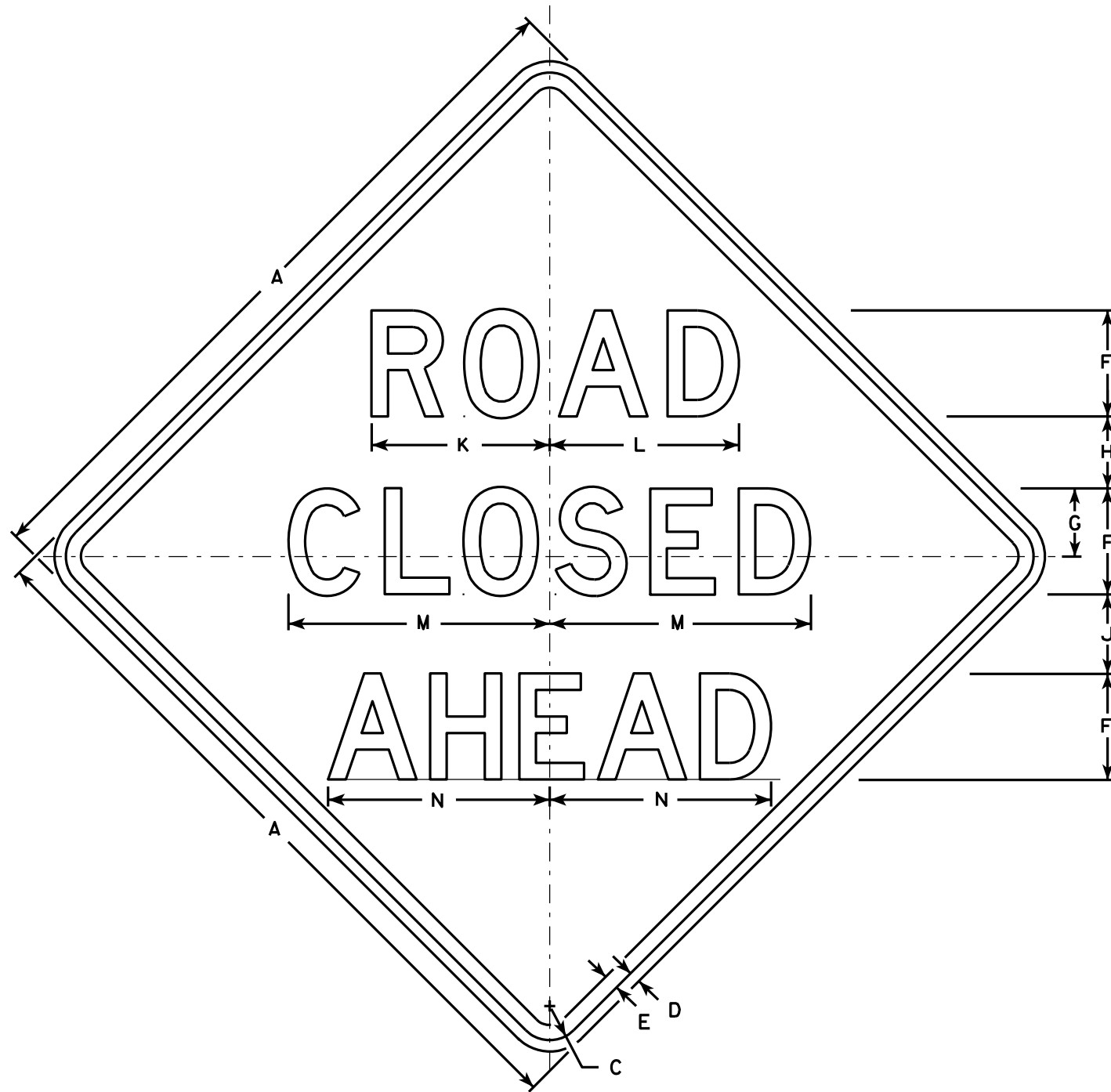
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/07/15 PLATE NO. W20-1.10



W20-3A

500 FT

W20-3D

1000 FT

W20-3C

1500 FT

W20-3B

1/2 MILE

W20-3G

1 MILE

W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

PROJECT NO:

HWY:

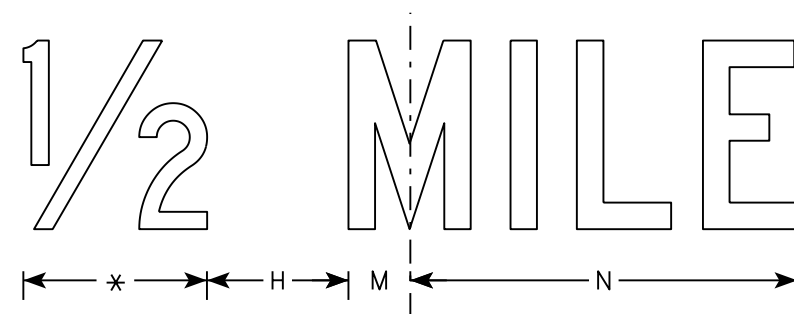
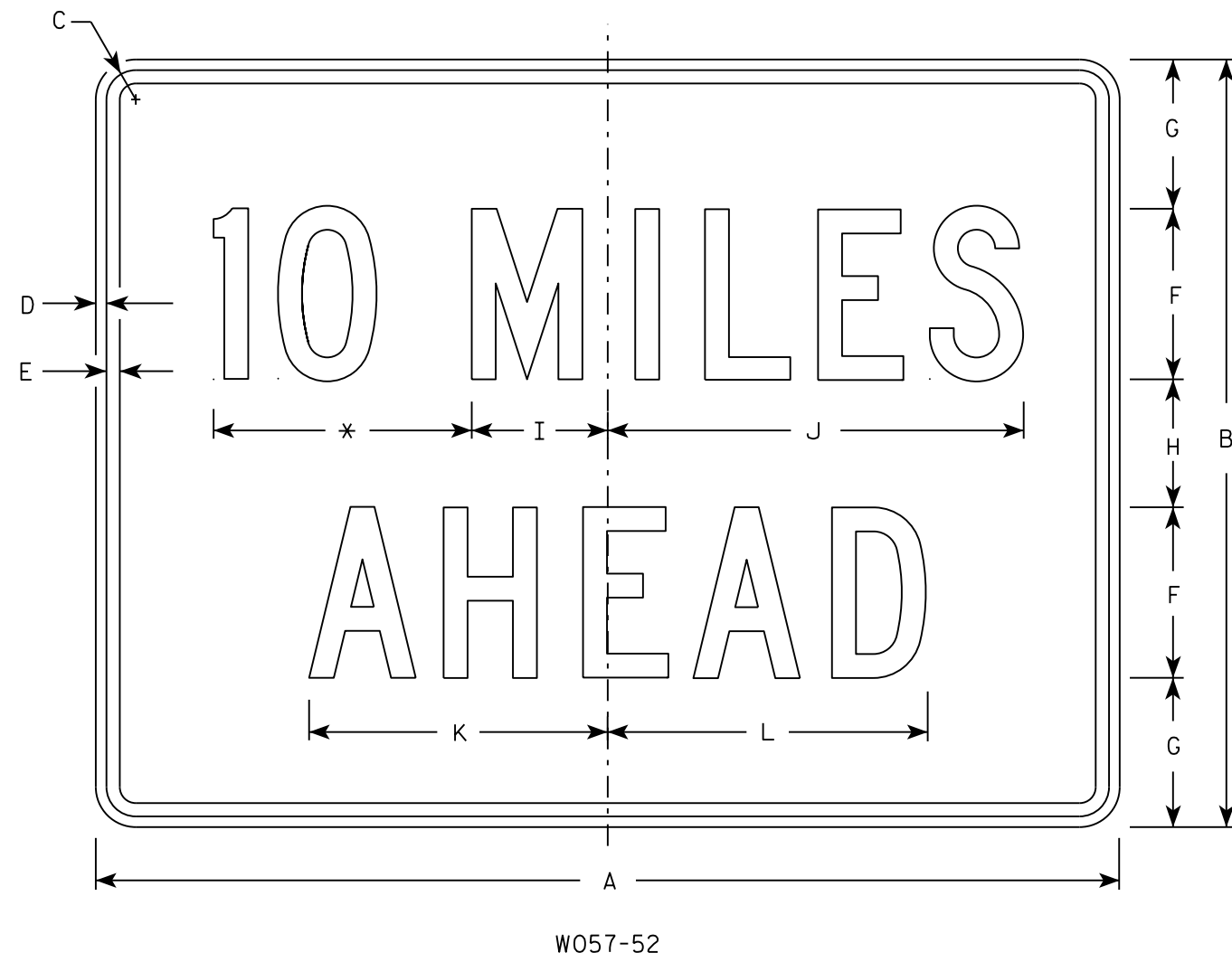
COUNTY:

SHEET NO:

E

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals to the nearest quarter mile and optically adjust spacing to achieve proper balance.

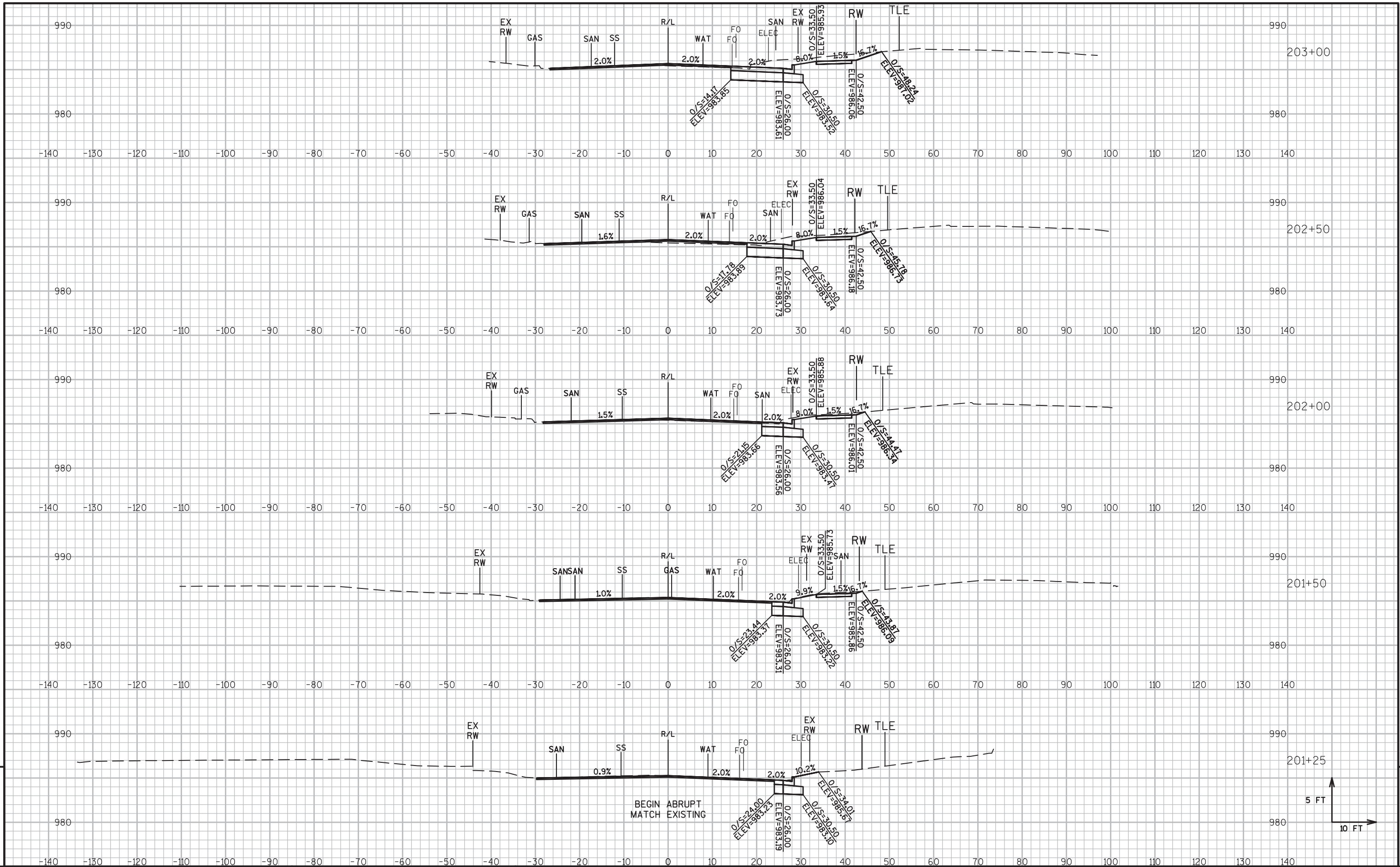


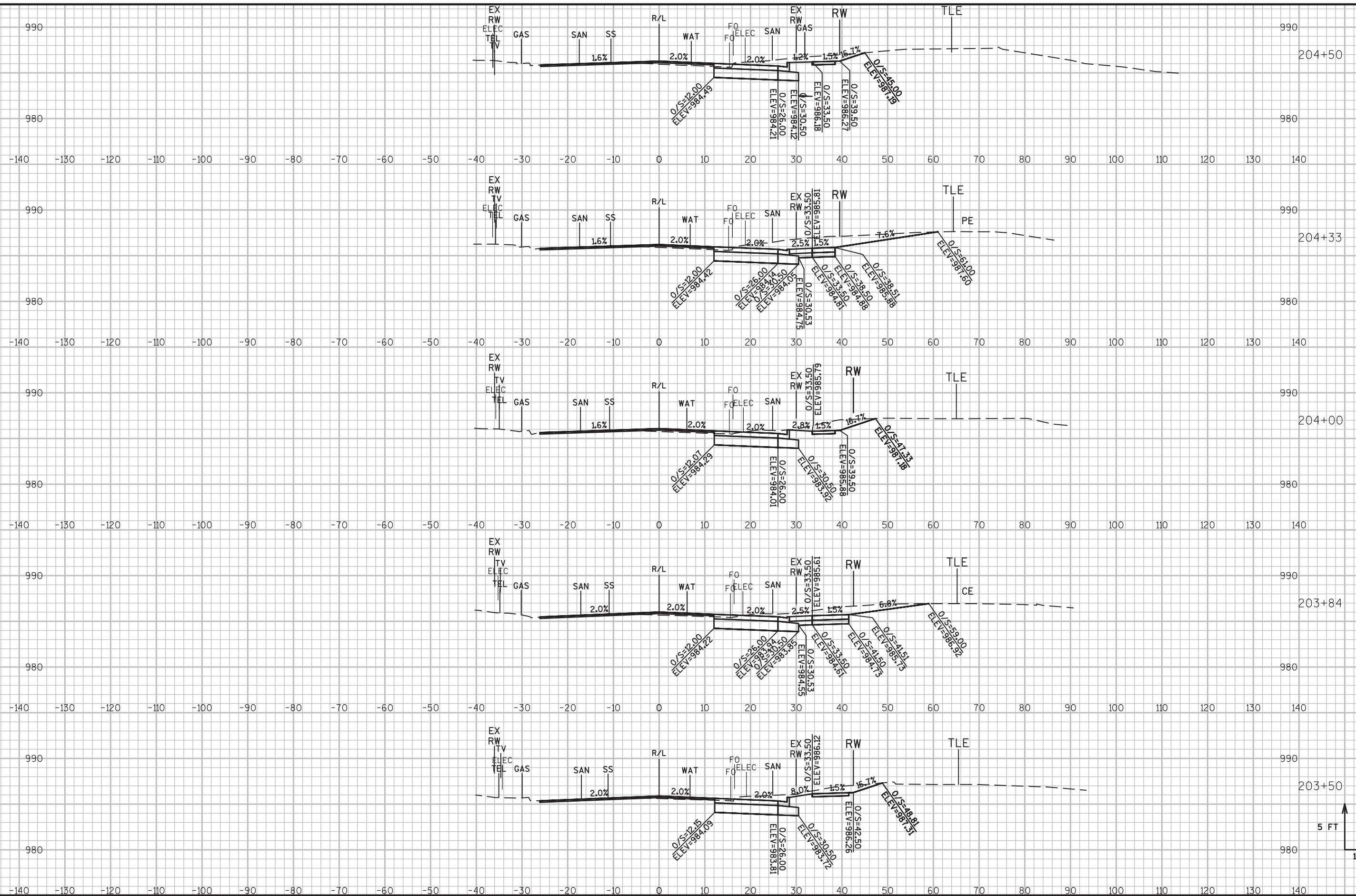
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	24	1 1/8	3/8	1/2	6	4 1/2	3	4 3/4	14 5/8	10 5/8	11 3/8	2	12													6.0
2S	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
2M	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
3	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
4	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0
5	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	16 3/8													12.0

STANDARD SIGN W057-52	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 3/21/17	PLATE NO. W057-52.2

STH 54 WIDENING								
STATION	DISTANCE	AREA SF		INCREMENTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		MASS ORDINATE
		CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	
				NOTE 1	NOTE 2	1.00 NOTE 1	1.25 NOTE 3	
201+25	0	12	0	0	0	0	0	0
201+50	25	17	0	14	0	14	0	14
202+00	50	23	0	38	0	52	0	52
202+50	50	32	0	51	0	103	0	103
203+00	50	46	0	73	0	176	0	176
203+50	50	47	0	87	0	263	0	263
203+84	34	52	0	63	0	326	0	326
204+00	16	44	0	29	0	355	0	355
204+33	33	64	0	66	0	421	0	421
204+50	17	45	0	35	0	456	0	456
205+00	50	49	0	88	0	544	0	544
205+50	50	56	0	98	0	642	0	642
206+00	50	65	0	113	0	755	0	755
206+50	50	52	0	109	0	864	0	864
206+72	22	71	0	51	0	915	0	915
207+00	28	67	0	72	0	987	0	987
207+21	21	69	0	53	0	1,040	0	1,040
207+50	29	34	0	56	0	1,096	0	1,096
208+00	50	29	1	59	1	1,155	2	1,153
208+50	50	21	2	47	3	1,202	5	1,197
209+00	50	20	1	38	3	1,240	9	1,231
209+50	50	17	1	35	2	1,275	12	1,263
209+84	34	15	1	21	2	1,296	14	1,282
210+00	16	14	1	9	1	1,305	15	1,290
210+50	50	11	1	24	2	1,329	18	1,311
210+59	9	10	1	4	1	1,333	19	1,314
COLUMN TOTALS				1,333	15			

NOTES:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL. SALVAGED UNUSABLE PAVEMENT MATERIAL IS CALCULATED ON THE SUMMARY SHEET.
2 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME, SALVAGED UNUSABLE PAVEMENT MATERIAL IS CALCULATED ON THE SUMMARY SHEET.
3 - EXPANDED FILL	(UNEXPANDED FILL) * (FILL FACTOR)
4 - MASS ORDINATE	CUT - (EXPANDED FILL); PLUS INDICATES AN EXCESS OF MATERIAL.





PROJECT NO: 6999-03-70

HWY: STH 54

COUNTY: WOOD

CROSS SECTIONS: STH 54

SHEET

FILE NAME : P:\48XX\4898.DP.15.STH54.WOOD\CADD5\69990370\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - *****

LAYOUT NAME - ####

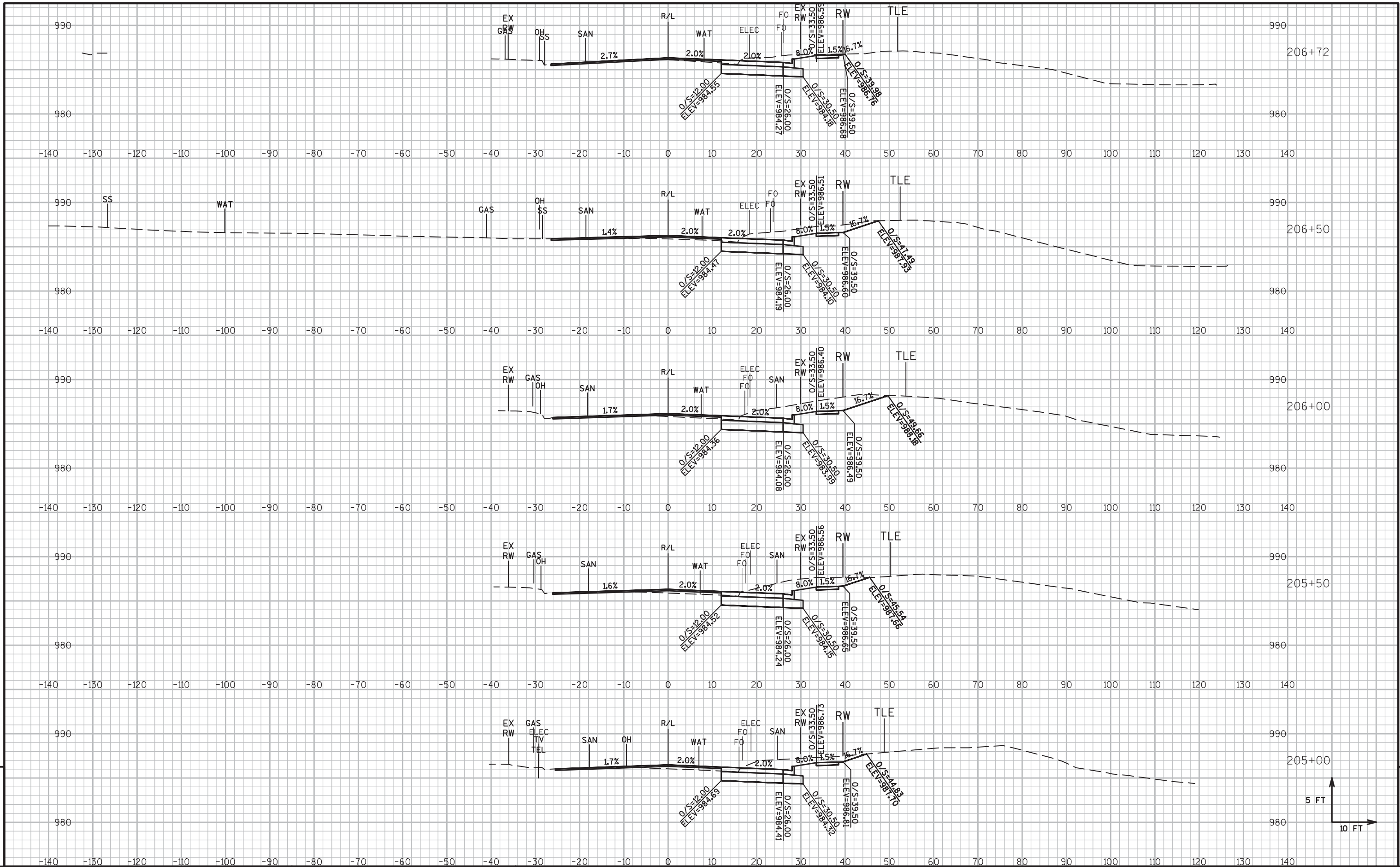
PLOT DATE : 7/18/2017 7:57 AM

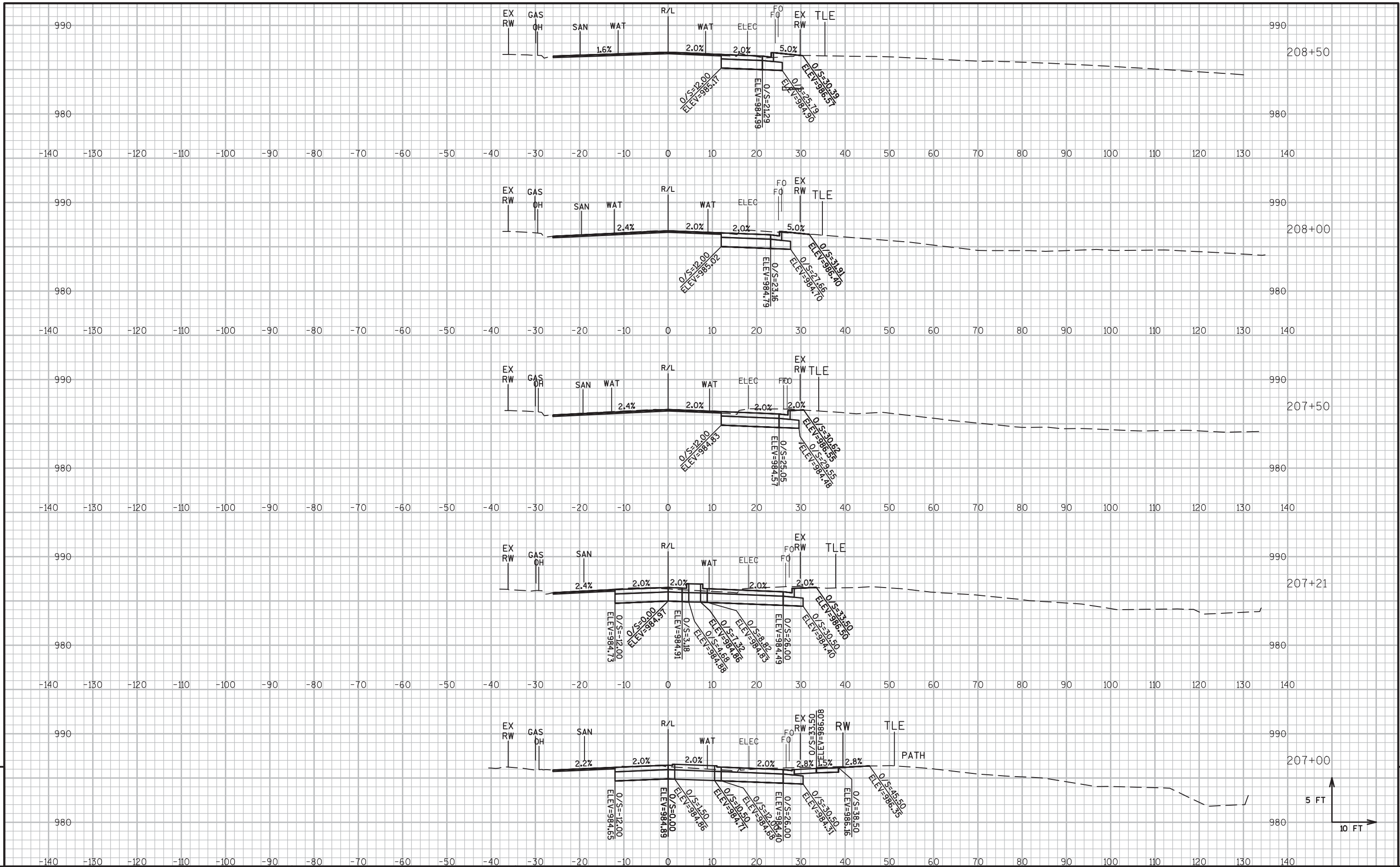
PLOT BY : MICHAEL HIGGINS

PLOT NAME :

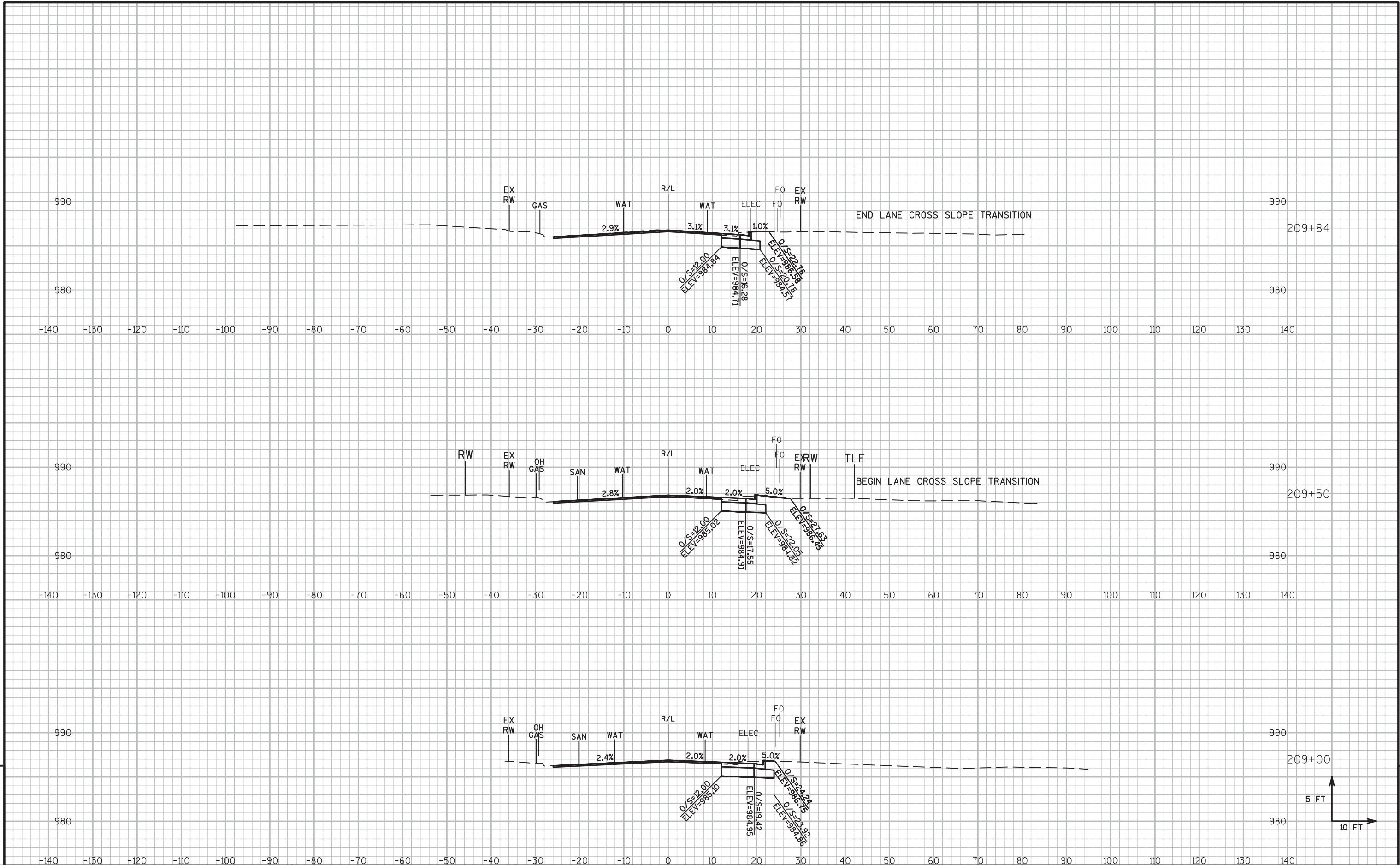
PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 49





9



9



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>