

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

UTILITY CONTACTS

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DNR LIAISON

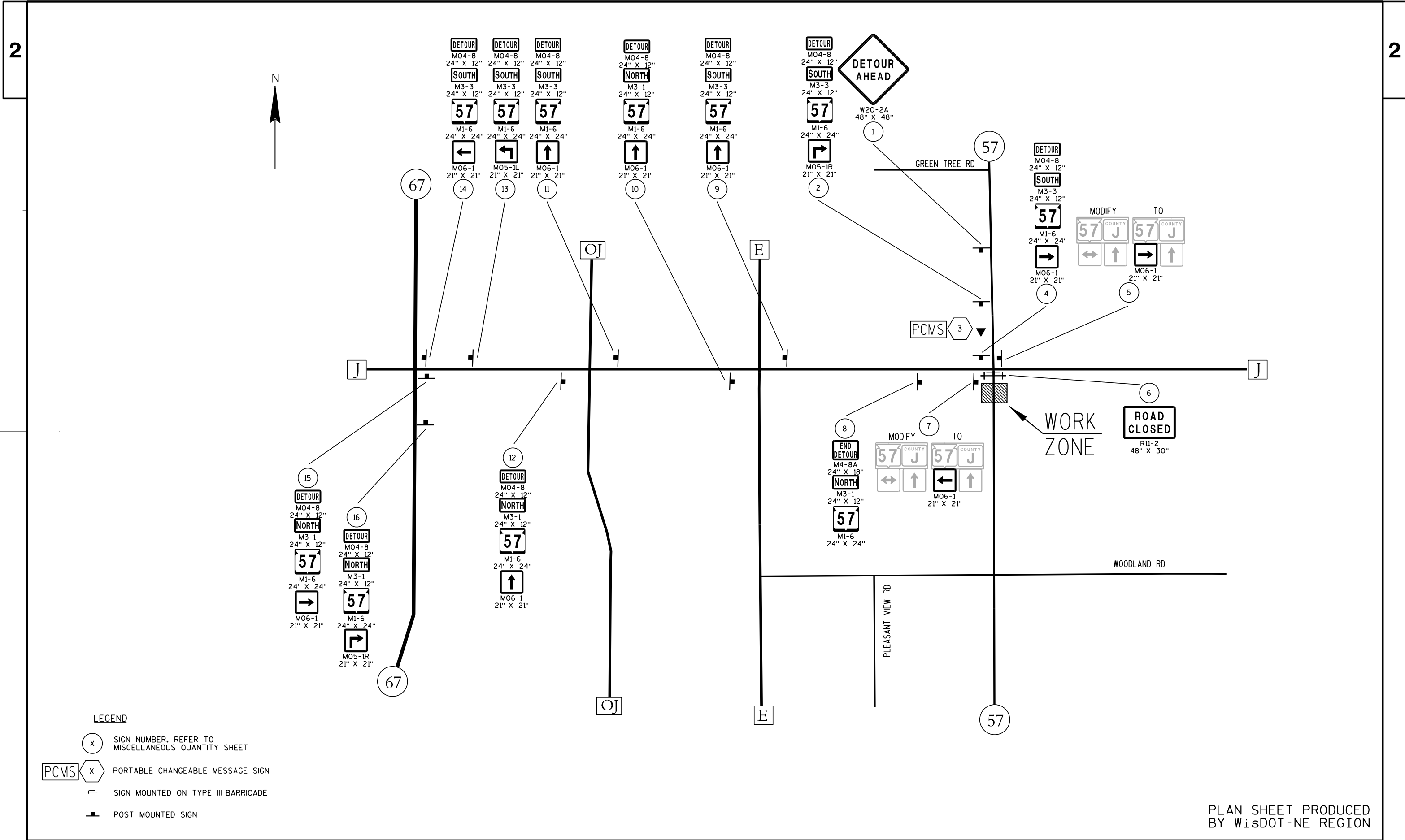
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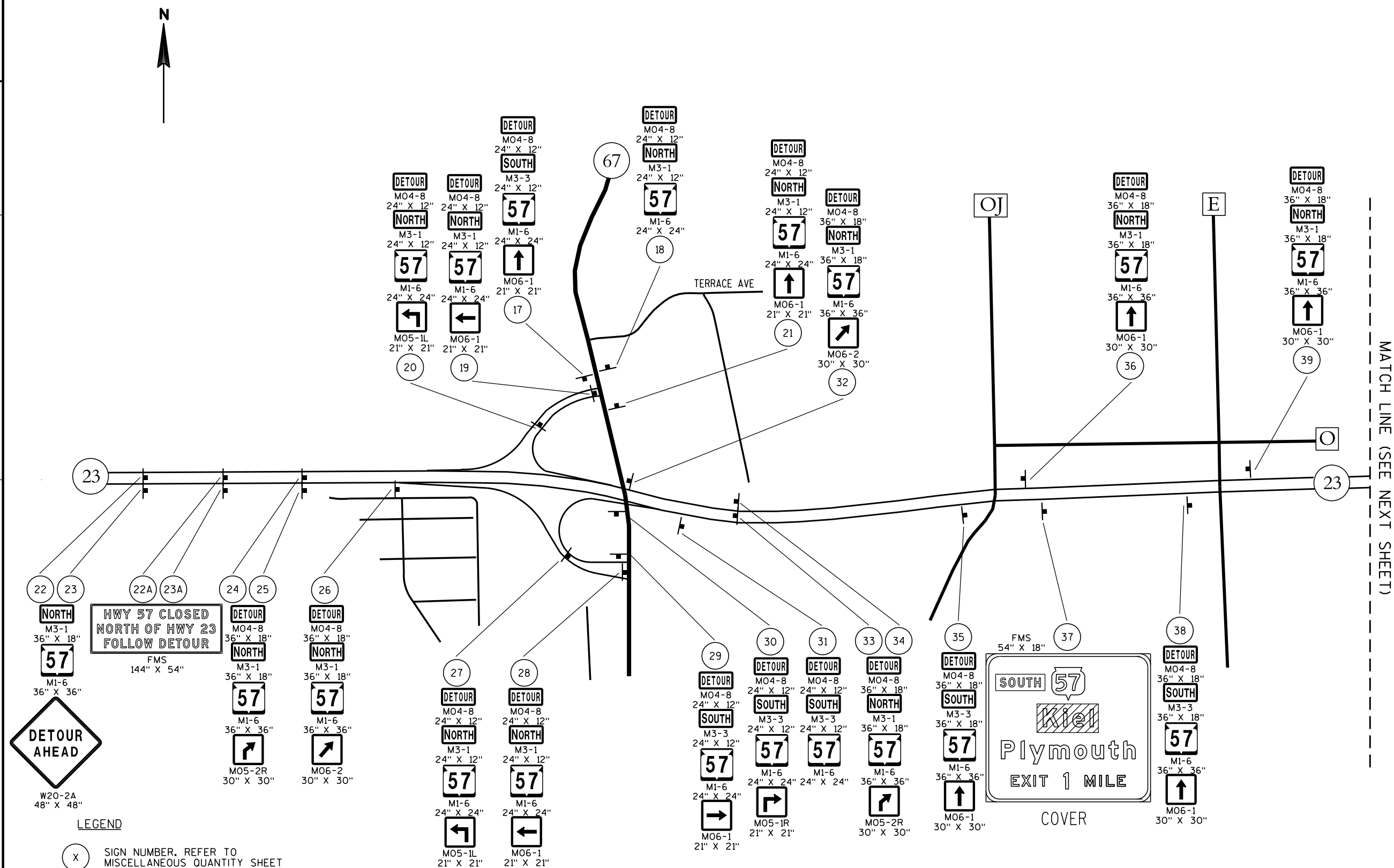
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Dial 811 or (800)242-8511
www.DiggersHotline.com





MATCH LINE (SEE NEXT SHEET)

LEGEND

- (X) SIGN NUMBER. REFER TO MISCELLANEOUS QUANTITY SHEET
- PCMS (X) PORTABLE CHANGEABLE MESSAGE SIGN
- ⇄ SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

PLAN SHEET PRODUCED
BY WISDOT-NE REGION

PROJECT NO: 4015-21-71

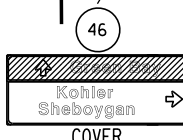
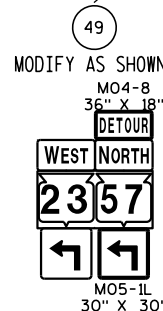
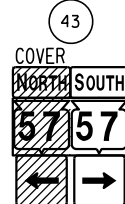
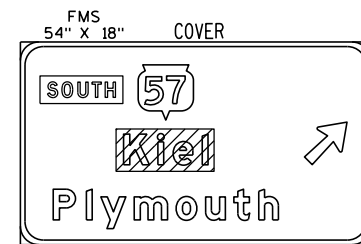
HWY: STH 57

COUNTY: SHEBOYGAN

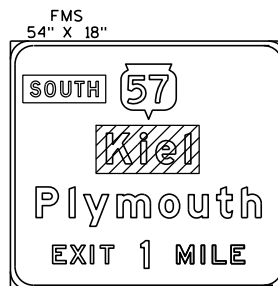
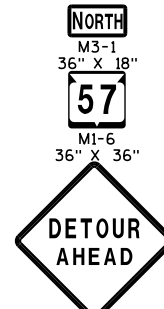
DETOUR SIGNING DETAIL FOR STRUCTURE B-59-0005

SHEET

E



57



MATCH LINE (SEE PREVIOUS SHEET)

WORK ZONE

PLEASANT VIEW RD

WOODLAND RD

WILLOW RD

M

J

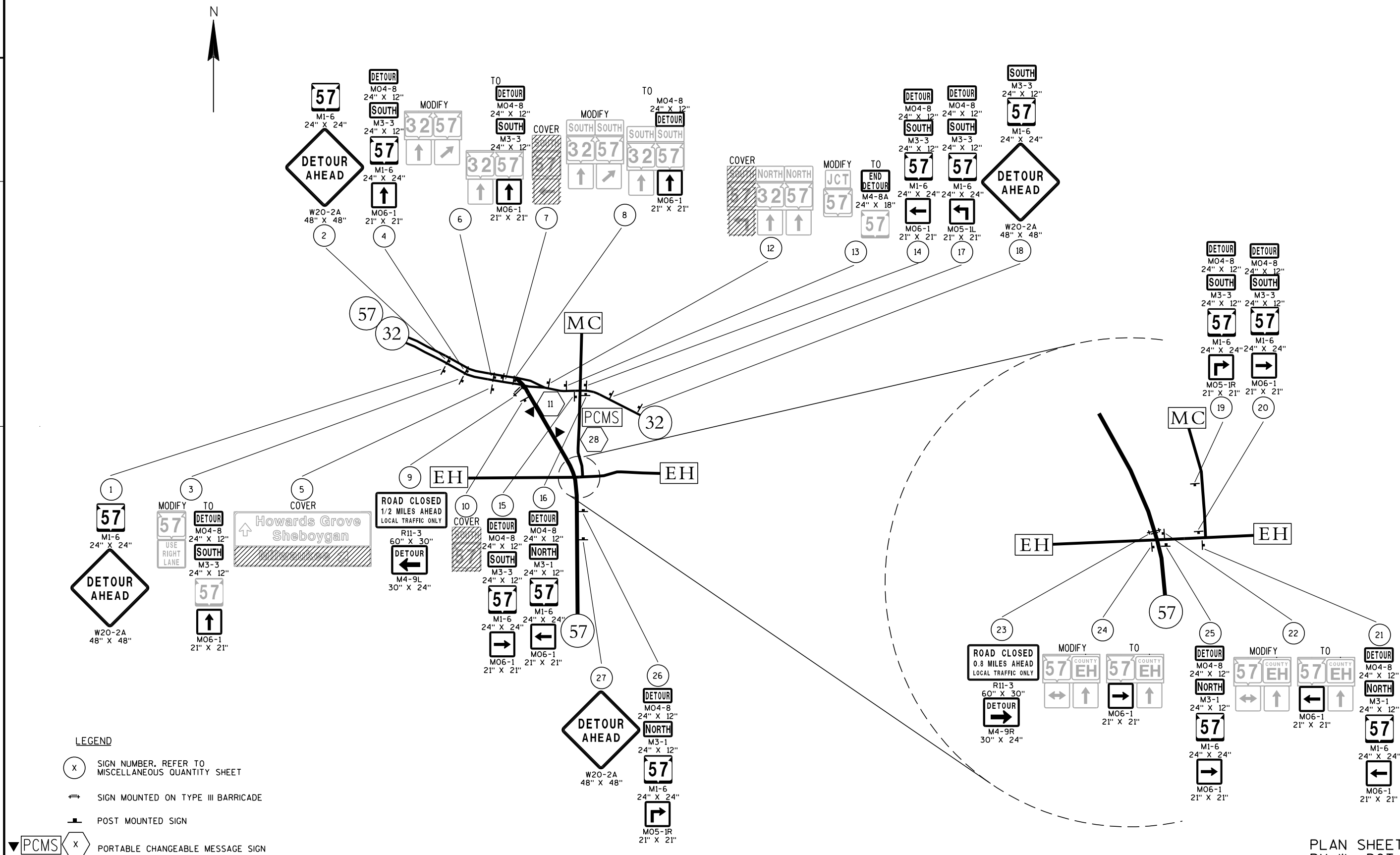
O

C

23

M

PLAN SHEET PRODUCED
BY WisDOT-NE REGION



Estimate Of Quantities

4015-21-71

| Line | Item | Item Description | Unit | Total | Qty |
|------|------------|-------------------------------------------------------------------------------------------|------|-----------|-----------|
| 0002 | 509.5100.S | Polymer Overlay | SY | 199.000 | 199.000 |
| 0004 | 517.1800.S | Structure Repainting Recycled Abrasive (structure) 01. B-59-0005 | LS | 1.000 | 1.000 |
| 0006 | 517.1800.S | Structure Repainting Recycled Abrasive (structure) 02. B-59-0006 | LS | 1.000 | 1.000 |
| 0008 | 517.4500.S | Negative Pressure Containment and Collection of Waste Materials (structure) 01. B-59-0005 | LS | 1.000 | 1.000 |
| 0010 | 517.4500.S | Negative Pressure Containment and Collection of Waste Materials (structure) 02. B-59-0006 | LS | 1.000 | 1.000 |
| 0012 | 517.6001.S | Portable Decontamination Facility | EACH | 2.000 | 2.000 |
| 0014 | 618.0100 | Maintenance And Repair of Haul Roads (project) 01. 4015-21-71 | EACH | 1.000 | 1.000 |
| 0016 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0018 | 642.5001 | Field Office Type B | EACH | 1.000 | 1.000 |
| 0020 | 643.0420 | Traffic Control Barricades Type III | DAY | 392.000 | 392.000 |
| 0022 | 643.0705 | Traffic Control Warning Lights Type A | DAY | 608.000 | 608.000 |
| 0024 | 643.0900 | Traffic Control Signs | DAY | 2,634.000 | 2,634.000 |
| 0026 | 643.0910 | Traffic Control Covering Signs Type I | EACH | 4.000 | 4.000 |
| 0028 | 643.0920 | Traffic Control Covering Signs Type II | EACH | 11.000 | 11.000 |
| 0030 | 643.1000 | Traffic Control Signs Fixed Message | SF | 135.000 | 135.000 |
| 0032 | 643.1050 | Traffic Control Signs PCMS | DAY | 28.000 | 28.000 |
| 0034 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |
| 0036 | 646.1020 | Marking Line Epoxy 4-Inch | LF | 97.500 | 97.500 |
| 0038 | ASP.1T0A | On-the-Job Training Apprentice at \$5.00/HR | HRS | 150.000 | 150.000 |
| 0040 | ASP.1T0G | On-the-Job Training Graduate at \$5.00/HR | HRS | 300.000 | 300.000 |

| TRAFFIC CONTROL SUMMARY - B-59-0006 SHEBOYGAN RIVER | | | | | | | | | | | | | | | | | | |
|-----------------------------------------------------|---------------------------|----------------------------|-------------------------------------------|------------|--------------------------------------------|------------|--------------------------|------------|---------------------------------------------|----------|----------------------------------------------|----------|-------------------------------------------|----------|----------------------------------|-----------|-----------------|---------------------------------|
| CATEGORY | LOCATION | APPROX. SERVICE DAYS | <u>643.0420</u> BARRICADES TYPE III | | <u>643.0705</u> WARNING LIGHT TYPE A | | <u>643.0900</u> SIGNS | | <u>643.0910</u> COVERING SIGNS TYPE I | | <u>643.0920</u> COVERING SIGNS TYPE II | | <u>643.1000</u> SIGNS FIXED MESSAGE | | <u>643.1050</u> SIGNS PCMS | | | REMARKS |
| | | | NO. IN SERVICE | DAYS | NO. IN SERVICE | DAYS | NO. IN SERVICE | DAYS | NO. IN SERVICE | EACH | NO. IN SERVICE | EACH | NO. IN SERVICE | S.F. | NO. IN SERVICE | DAYS | NO OF CYCLES | |
| 0030 | STH 57 - SOUTH OF BRIDGE | 12 | 7 | 84 | 10 | 120 | 2 | 24 | | 0 | | 0 | | 0 | | 0 | | BRIDGE APPROACH |
| 0030 | STH 57 - NORTH OF BRIDGE | 12 | 7 | 84 | 10 | 120 | 2 | 24 | | 0 | | 0 | | 0 | | 0 | | BRIDGE APPROACH |
| 0030 | STH 57 DETOUR - B-59-0006 | 12 | 2 | 24 | 4 | 48 | 63 | 756 | | 0 | 4 | 4 | | 0 | 2 | 14 | 1 | *DETAILS IN DETOUR SIGN SUMMARY |
| SUBTOTAL | | | | 192 | | 288 | | 804 | | 0 | | 4 | | 0 | | 14 | | |

| | | | | | | | |
|----------------|-----|-----|-------|---|----|-----|----|
| PROJECT TOTALS | 392 | 608 | 2,634 | 4 | 11 | 135 | 28 |
|----------------|-----|-----|-------|---|----|-----|----|

TRAFFIC CONTROL DETOUR SIGN SUMMARY - STRUCTURE B-59-0005

| SIGN NO. | LOCATION | SIGN CODE | SIZE W X H | NUMBER IN SERVICE | APPROX. SERVICE PERIOD 10 DAYS | 643.0900 SIGNS DAYS | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.1000 FIXED MESSAGE SIGN S. F. | 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS | NO OF CYCLES | 643.0910 COVERING SIGNS TYPE I EACH | 643.0920 COVERING SIGNS TYPE II EACH | REMARKS |
|----------------|--------------------------------------------------------------------------------|-----------|------------|-------------------|--------------------------------|---------------------|-----------------------------------|-------------------------------------|-----------------------------------|-------------------------------------------------|--------------|-------------------------------------|--------------------------------------|---------|
| 1 | STH 57, PLACE 1200' N. OF CTH J | W 20-2A | 48"X48" | 1 | 10 | 10 | | | | | | | | |
| 2 | STH 57, PLACE 600' N. OF CTH J | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-1R | 21"X21" | 1 | 10 | 10 | | | | | | | | |
| 3 | STH 57, PLACE N. OF CTH J ON RIGHT SHOULDER, FIELD DETERMINED | PCMS | | 1 | | | | | | 7 | | | | |
| 4 | STH 57, AT CTH J INTERSECTION, PLACE TO RIGHT OF EXISTING J13-1 SIGN | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | RIGHT |
| 5 | CTH J, AT STH 57 INTERSECTION, MODIFY EXISTING J13-2 AS SHOWN | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | RIGHT |
| 6 | STH 57, AT CTH J INTERSECTION, PLACE IN SW QUADRANT OF INTERSECTION IN SB LANE | R 11-2 | 48"X30" | 1 | 10 | 10 | 10 | 20 | | | | | | |
| 7 | CTH J, AT STH 57 INTERSECTION, MODIFY EXISTING J13-2 AS SHOWN | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | LEFT |
| 8 | CTH J, W. OF STH 57, PLACE 1000' W. OF STH 57 INTERSECTION | M 4-8A | 24"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| 9 | CTH J, E. OF CTH E, PLACE 100' E. OF CTH E INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 10 | CTH J, W. OF CTH E, PLACE 100' W. OF CTH E INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 11 | CTH J, E. OF CTH E, PLACE 100' E. OF CTH OJ INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 12 | CTH J, W. OF CTH E, PLACE 100' W. OF CTH OJ INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 13 | CTH J, E. OF STH 67, PLACE 600' E. OF STH 67 INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-1L | 21"X21" | 1 | 10 | 10 | | | | | | | | |
| 14 | CTH J, AT STH 67 INTERSECTION, PLACE 100' E. OF STH 67 INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | LEFT |
| PAGE SUBTOTALS | | | | 40 | | 390 | 10 | 20 | 0 | 7 | | 0 | 0 | |

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

TRAFFIC CONTROL DETOUR SIGN SUMMARY - STRUCTURE B-59-0005

| SIGN NO. | LOCATION | SIGN CODE | SIZE W X H | NUMBER IN SERVICE | APPROX. SERVICE PERIOD 10 DAYS | 643.0900 SIGNS DAYS | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.1000 FIXED MESSAGE SIGN S.F. | 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS | NO OF CYCLES | 643.0910 COVERING SIGNS TYPE I EACH | 643.0920 COVERING SIGNS TYPE II EACH | REMARKS |
|----------------|---------------------------------------------------------------------------------|-----------|------------|-------------------|--------------------------------|---------------------|-----------------------------------|-------------------------------------|----------------------------------|-------------------------------------------------|--------------|-------------------------------------|--------------------------------------|---------|
| 15 | STH 67, AT CTH J INTERSECTION, PLACE RIGHT OF EXISTING J13-1 SIGN | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | RIGHT |
| 16 | STH 67, S. OF CTH J, PLACE 600' S. OF CTH 67 INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-1R | 21"X21" | 1 | 10 | 10 | | | | | | | | |
| 17 | STH 67, N. OF STH 23 WB RAMP INTERSECTION, PLACE 100' N. OF RAMP INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 18 | STH 67, N. OF STH 23 WB RAMP INTERSECTION, PLACE 100' N. OF RAMP INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| 19 | STH 23 WB OFF-RAMP TO STH 67, PLACE RIGHT OF EXISTING R5-1A SIGN ON RAMP | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | LEFT |
| 20 | STH 23 WB OFF-RAMP TO STH 67, PLACE 600' PRIOR TO STH 67 ON LEFT SIDE | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-1L | 21"X21" | 1 | 10 | 10 | | | | | | | | |
| 21 | STH 67, S. OF STH 23 WB RAMP INTERSECTION, PLACE 250' S. OF RAMP INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 22 | STH 23, W. OF STH 67 INTERCHANGE, PLACE 2640' W. OF OFF-RAMP TO STH 67 - MEDIAN | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | W 20-2A | 48"X48" | 1 | 10 | 10 | | | | | | | | |
| 22A | STH 23, W. OF STH 67 INTERCHANGE, PLACE 1500' W. OF OFF-RAMP TO STH 67 - MEDIAN | FMS | 144"X54" | 1 | | | | | 54 | | | | | |
| 23 | STH 23, W. OF STH 67 INTERCHANGE, PLACE 2640' W. OF OFF-RAMP TO STH 67 - RIGHT | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | W 20-2A | 48"X48" | 1 | 10 | 10 | | | | | | | | |
| 23A | STH 23, W. OF STH 67 INTERCHANGE, PLACE 1500' W. OF OFF-RAMP TO STH 67 - RIGHT | FMS | 144"X54" | 1 | | | | | 54 | | | | | |
| 24 | STH 23, W. OF STH 67 INTERCHANGE, PLACE 750' W. OF OFF-RAMP TO STH 67 - MEDIAN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-2R | 30"X30" | 1 | 10 | 10 | | | | | | | | |
| PAGE SUBTOTALS | | | | 39 | | 370 | 0 | 0 | 108 | 0 | | 0 | 0 | |

PLAN SHEET PRODUCED

BY WisDOT - NE REGION

PROJECT NUMBER: 4015-21-71

HWY: STH 57

COUNTY: SHEBOYGAN

MISCELLANEOUS QUANTITIES FOR STRUCTURE B-59-0005

SHEET

E

TRAFFIC CONTROL DETOUR SIGN SUMMARY - STRUCTURE B-59-0005

| SIGN NO. | LOCATION | SIGN CODE | SIZE W X H | NUMBER IN SERVICE | APPROX. SERVICE PERIOD 10 DAYS | 643.0900 SIGNS DAYS | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.1000 FIXED MESSAGE SIGN S.F. | 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS | NO OF CYCLES | 643.0910 COVERING SIGNS TYPE I EACH | 643.0920 COVERING SIGNS TYPE II EACH | REMARKS |
|----------------|--------------------------------------------------------------------------------|-----------|------------|-------------------|--------------------------------|---------------------|-----------------------------------|-------------------------------------|----------------------------------|-------------------------------------------------|--------------|-------------------------------------|--------------------------------------|------------|
| 25 | STH 23, W. OF STH 67 INTERCHANGE, PLACE 750' W. OF OFF-RAMP TO STH 67 - RIGHT | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-2R | 30"X30" | 1 | 10 | 10 | | | | | | | | |
| 26 | STH 23, AT STH 67 INTERCHANGE OFF-RAMP, PLACE LEFT OF EXISTING E4-1A SIGN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-2 | 30"X30" | 1 | 10 | 10 | | | | | | | | TILT RIGHT |
| 27 | STH 23 WB OFF-RAMP TO STH 67, PLACE 600' PRIOR TO STH 67 ON LEFT SIDE | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-1L | 21"X21" | 1 | 10 | 10 | | | | | | | | |
| 28 | STH 23 WB OFF-RAMP TO STH 67, PLACE RIGHT OF EXISTING R5-1A SIGN ON RAMP | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | LEFT |
| 29 | STH 67, AT STH 23 EB RAMP INTERSECTION, PLACE RIGHT OF EXISTING J3-1 SIGN | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 10 | 10 | | | | | | | | RIGHT |
| 30 | STH 67, N. OF STH 23 EB RAMP INTERSECTION, PLACE 500' N. OF RAMP INTERSECTION | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-1R | 21"X21" | 1 | 10 | 10 | | | | | | | | |
| 31 | STH 23, E. OF STH 67, PLACE 1000' E. OF STH 67 STRUCTURE | MO 4-8 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 10 | 10 | | | | | | | | 57 |
| 32 | STH 23, AT STH 67 INTERCHANGE OFF-RAMP, PLACE LEFT OF EXISTING E4-1A SIGN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-2 | 30"X30" | 1 | 10 | 10 | | | | | | | | TILT RIGHT |
| 33 | STH 23, E. OF STH 67 INTERCHANGE, PLACE 750' E. OF OFF-RAMP TO STH 67 - MEDIAN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-2R | 30"X30" | 1 | 10 | 10 | | | | | | | | |
| 34 | STH 23, E. OF STH 67 INTERCHANGE, PLACE 750' E. OF OFF-RAMP TO STH 67 - RIGHT | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 5-2R | 30"X30" | 1 | 10 | 10 | | | | | | | | |
| PAGE SUBTOTALS | | | | 39 | | 390 | 0 | 0 | 0 | 0 | | 0 | 0 | |

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

PROJECT NUMBER: 4015-21-71

HWY: STH 57

COUNTY: SHEBOYGAN

MISCELLANEOUS QUANTITIES FOR STRUCTURE B-59-0005

SHEET

E

TRAFFIC CONTROL DETOUR SIGN SUMMARY - STRUCTURE B-59-0005

| SIGN NO. | LOCATION | SIGN CODE | SIZE W X H | NUMBER IN SERVICE | APPROX. SERVICE PERIOD 10 DAYS | 643.0900 SIGNS DAYS | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.1000 FIXED MESSAGE SIGN S.F. | 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS | NO OF CYCLES | 643.0910 COVERING SIGNS TYPE I EACH | 643.0920 COVERING SIGNS TYPE II EACH | REMARKS |
|----------------|------------------------------------------------------------------------------------|-----------|------------|-------------------|--------------------------------|---------------------|-----------------------------------|-------------------------------------|----------------------------------|-------------------------------------------------|--------------|-------------------------------------|--------------------------------------|-------------------|
| 35 | STH 23, W. OF CTH O, PLACE LEFT OF EXISTING J3-1 SIGN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 36 | STH 23, E. OF CTH O, PLACE RIGHT OF EXISTING J3-2 SIGN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 37 | STH 23, E. OF CTH O, COVER/MODIFY EXISTING TYPE I SIGN AS SHOWN | | | 1 | | | | | | | 1 | 1 | | COVER "KIEL " |
| | " | FMS | 54"X18" | 1 | | | | | 6.75 | | | | | "SOUTH" |
| 38 | STH 23, W. OF CTH E, PLACE LEFT OF EXISTING J3-1 SIGN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 39 | STH 23, E. OF CTH E, PLACE RIGHT OF EXISTING J3-2 SIGN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 40 | STH 23, AT STH 57 INTERCHANGE OFF-RAMP, COVER/MODIFY EXISTING TYPE I SIGN AS SHOWN | | | 1 | | | | | | | 1 | 1 | | COVER "KIEL " |
| | " | FMS | 54"X18" | 1 | | | | | 6.75 | | | | | "SOUTH" |
| 41 | STH 23, AT STH 57 INTERCHANGE OFF-RAMP, PLACE LEFT OF EXISTING E4-1A SIGN | M 4-8A | 30"X24" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-3 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| 42 | STH 23 EB OFF-RAMP TO STH 57, COVER EXISTING D1-3 SIGN AS SHOWN | | | 1 | | | | | | | 1 | | 1 | COVER "GREEN BAY" |
| 43 | STH 23 EB OFF-RAMP TO STH 57, COVER EXISTING J3-2 SIGN AS SHOWN | | | 1 | | | | | | | 1 | | 1 | COVER "N 57 LT" |
| 44 | STH 57, N. OF CTH C, PLACE 500' N. OF CTH C INTERSECTION - MEDIAN | W 20-2A | 48"X48" | 1 | 10 | 10 | | | | | | | | |
| 45 | STH 57, N. OF CTH C, PLACE 500' N. OF CTH C INTERSECTION - RIGHT | W 20-2A | 48"X48" | 1 | 10 | 10 | | | | | | | | |
| 46 | STH 57, S. OF STH 23 EB RAMP INTERCHANGE, COVER EXISTING D1-3 AS SHOWN | | | 1 | | | | | | | 1 | | 1 | COVER "GREEN BAY" |
| 47 | STH 57, S. OF STH 23 EB RAMP INTERSECTION, PLACE 300' S. OF INTERSECTION - MEDIAN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 48 | STH 57, S. OF STH 23 EB RAMP INTERSECTION, PLACE 300' S. OF INTERSECTION - RIGHT | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| PAGE SUBTOTALS | | | | 36 | | 290 | 0 | 0 | 13.50 | 0 | | 2 | 3 | |

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

PROJECT NUMBER: 4015-21-71

HWY: STH 57

COUNTY: SHEBOYGAN

MISCELLANEOUS QUANTITIES FOR STRUCTURE B-59-0005

SHEET

E

TRAFFIC CONTROL DETOUR SIGN SUMMARY - STRUCTURE B-59-0005

| SIGN NO. | LOCATION | SIGN CODE | SIZE W X H | NUMBER IN SERVICE | APPROX. SERVICE PERIOD 10 DAYS | 643.0900 SIGNS DAYS | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.1000 FIXED MESSAGE SIGN S.F. | 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS | NO OF CYCLES | 643.0910 COVERING SIGNS TYPE I EACH | 643.0920 COVERING SIGNS TYPE II EACH | REMARKS |
|----------------------------|-----------------------------------------------------------------------------------|-----------|------------|-------------------|--------------------------------|---------------------|-----------------------------------|-------------------------------------|----------------------------------|-------------------------------------------------|--------------|-------------------------------------|--------------------------------------|-------------------|
| 49 | STH 57, N. OF STH 23 EB RAMP INTERSECTION, MODIFY EXISTING J2-2 SIGN AS SHOWN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | MO 5-1L | 30"X30" | 1 | 10 | 10 | | | | | | | | |
| 50 | STH 57, S. OF STH 23 WB RAMP INTERSECTION, PLACE 300' S. OF INTERSECTION - MEDIAN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | LEFT |
| 51 | STH 57, N. OF STH 23 WB RAMP INTERSECTION, PLACE IN NE QUADRANT IN RIGHT LANE | R 11-3 | 60"X30" | 1 | 10 | 10 | 10 | 20 | | | | | | 2.2 MILES AHEAD |
| | " | M 4-9L | 30"X24" | 1 | 10 | 10 | | | | | | | | |
| 52 | STH 57, N. OF STH 23 WB RAMP INTERSECTION, COVER EXISTING J4-1 SIGN AS SHOWN | | | 1 | | | | | | | 1 | | 1 | COVER "NORTH 57" |
| 53 | STH 57, PLACE S. OF OTTER CREEK STRUCTURE ON RIGHT SHOULDER, FIELD DETERMINED | PCMS | | 1 | | | | | | 7 | | | | |
| 54 | STH 57, S. OF CTH J, COVER EXISTING J4-1 SIGN AS SHOWN | | | 1 | | | | | | | 1 | | 1 | COVER "SOUTH 57" |
| 55 | STH 23 WB OFF-RAMP TO STH 57, COVER EXISTING J3-1 SIGN AS SHOWN | | | 1 | | | | | | | 1 | | 1 | COVER "N 57 RT" |
| 56 | STH 23 WB OFF-RAMP TO STH 57, COVER EXISTING D1-3 SIGN AS SHOWN | | | 1 | | | | | | | 1 | | 1 | COVER "GREEN BAY" |
| 57 | STH 23, AT STH 57 INTERCHANGE OFF-RAMP, PLACE LEFT OF EXISTING E4-1A SIGN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 58 | STH 23, AT STH 57 INTERCHANGE OFF-RAMP, COVER/MODIFY EXISTING E4-1A SIGN AS SHOWN | | | 1 | | | | | | | 1 | 1 | | COVER "KIEL" |
| | " | FMS | 54"X18" | 1 | | | | | 6.75 | | | | | "SOUTH" |
| 59 | STH 23, E. OF STH 57 INTERCHANGE, PLACE 1320' E. OF STH 57 OFF-RAMP - RIGHT | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 60 | STH 23, E. OF STH 57 INTERCHANGE, PLACE 1320' E. OF STH 57 OFF-RAMP - MEDIAN | MO 4-8 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | MO 6-1 | 30"X30" | 1 | 10 | 10 | | | | | | | | AHEAD |
| 61 | STH 23, W. OF WILLOW RD, PLACE 1300' W. OF WILLOW RD - RIGHT | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | W 20-2A | 48"X48" | 1 | 10 | 10 | | | | | | | | |
| 62 | STH 23, W. OF WILLOW RD, PLACE 1300' W. OF WILLOW RD - MEDIAN | M 3-1 | 36"X18" | 1 | 10 | 10 | | | | | | | | |
| | " | M 1-6 | 36"X36" | 1 | 10 | 10 | | | | | | | | 57 |
| | " | W 20-2A | 48"X48" | 1 | 10 | 10 | | | | | | | | |
| 63 | STH 23, E. OF WILLOW RD, COVER/MODIFY EXISTING E1-51A AS SHOWN | | | 1 | | | | | | | 1 | 1 | | COVER "KIEL" |
| | " | FMS | 54"X18" | 1 | | | | | 6.75 | | | | | "SOUTH" |
| PAGE SUBTOTALS | | | | 35 | | 260 | 10 | 20 | 13.50 | 7 | | 2 | 4 | |
| STRUCTURE B-59-0005 TOTALS | | | | 189 | | 1,700 | 20 | 40 | 135.00 | 14 | | 4 | 7 | |

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

TRAFFIC CONTROL DETOUR SIGN SUMMARY - STRUCTURE B-59-0006

| SIGN NO. | LOCATION | SIGN CODE | SIZE W X H | NUMBER IN SERVICE | APPROX. SERVICE PERIOD 12 DAYS | 643.0900 SIGNS DAYS | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS | NO OF CYCLES | 643.0920 COVERING SIGNS TYPE II EACH | REMARKS |
|----------------|---------------------------------------------------------------------------------|-----------|------------|-------------------|--------------------------------|---------------------|-----------------------------------|-------------------------------------|-------------------------------------------------|--------------|--------------------------------------|----------------------------|
| 1 | STH 32/57, W. OF 32-57 SPLIT, PLACE 2000' PRIOR TO SPLIT (RIGHT SIDE) | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | W 20-2A | 48"X48" | 1 | 12 | 12 | | | | | | |
| 2 | STH 32/57, W. OF 32-57 SPLIT, PLACE 2000' PRIOR TO SPLIT (MEDIAN) | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | W 20-2A | 48"X48" | 1 | 12 | 12 | | | | | | |
| 3 | STH 32/57, W. OF 32-57 SPLIT, MODIFY EXISTING J12-1 AS SHOWN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | AHEAD |
| 4 | STH 32/57, W. OF 32-57 SPLIT, PLACE IN MEDIAN ACROSS FROM SIGN #3 | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | AHEAD |
| 5 | STH 32/57, W. OF 32-57 SPLIT, COVER EXISTING D1-3 SIGN AS SHOWN | | | 1 | | | | | | 1 | 1 | COVER "MILWAUKEE" |
| 6 | STH 32/57, W. OF 32-57 SPLIT, MODIFY EXISTING J13-2 AS SHOWN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | AHEAD |
| 7 | STH 32/57, W. OF 32-57 SPLIT, COVER EXISTING J3-1 SIGN AS SHOWN | | | 1 | | | | | | 1 | 1 | COVER "SOUTH 57 LEFT" |
| 8 | STH 32/57, AT 32-57 SPLIT, MODIFY EXISTING J3-1 SIGN AS SHOWN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | AHEAD |
| 9 | STH 32/57, AT 32-57 SPLIT, PLACE ON RIGHT SHOULDER AT THEORETICAL GORE MARKINGS | R 11-3 | 60"X30" | 1 | 12 | 12 | 12 | 24 | | | | 1/2 MILES AHEAD |
| | " | M 4-9L | 30"X24" | 1 | 12 | 12 | | | | | | |
| 10 | STH 57, S. OF 32-57 SPLIT, COVER EXISTING J4-1 SIGN AS SHOWN | | | 1 | | | | | | 1 | 1 | COVER "SOUTH 57" |
| 11 | STH 57, S. OF 32-57 SPLIT, PLACE N. OF SHEBOYGAN RIVER BRIDGE ON RIGHT SHOULDER | PCMS | | 1 | | | | | 7 | | | FIELD DETERMINED PLACEMENT |
| 12 | STH 32, E. OF 32-57 SPLIT, COVER EXISTING J3-3 SIGN AS SHOWN | | | 1 | | | | | | 1 | 1 | COVER "SOUTH 57 LEFT TURN" |
| 13 | STH 32, W. OF CTH MC, MODIFY EXISTING J1-1 AS SHOWN | M 4-8A | 24"X18" | 1 | 12 | 12 | | | | | | |
| 14 | STH 32, AT CTH MC INTERSECTION, PLACE LEFT OF EXISTING J3-1 SIGN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | LEFT |
| 15 | STH 32, AT CTH MC INTERSECTION, PLACE LEFT OF EXISTING J3-1 SIGN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | RIGHT |
| 16 | CTH MC, AT STH 32 INTERSECTION, PLACE RIGHT OF EXISTING J13-1 AND R1-1 SIGN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | LEFT |
| 17 | STH 32, E. OF CTH MC, PLACE 750' E. OF CTH MC INTERSECTION | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 5-1L | 21"X21" | 1 | 12 | 12 | | | | | | |
| 18 | STH 32, E. OF CTH MC, PLACE 1500' E. OF CTH MC INTERSECTION | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | W 20-2A | 48"X48" | 1 | 12 | 12 | | | | | | |
| PAGE SUBTOTALS | | | | 43 | | 456 | 12 | 24 | 7 | | 4 | |

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

TRAFFIC CONTROL DETOUR SIGN SUMMARY - STRUCTURE B-59-0006

| SIGN NO. | LOCATION | SIGN CODE | SIZE W X H | NUMBER IN SERVICE | APPROX. SERVICE PERIOD 12 DAYS | 643.0900 SIGNS DAYS | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS | NO OF CYCLES | 643.0920 COVERING SIGNS TYPE II EACH | REMARKS |
|----------------------------|---------------------------------------------------------------------------------|-----------|------------|-------------------|--------------------------------|---------------------|-----------------------------------|-------------------------------------|-------------------------------------------------|--------------|--------------------------------------|----------------------------|
| 19 | CTH MC, N. OF CTH EH, PLACE 750' N. OF CTH EH INTERSECTION | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 5-1R | 21"X21" | 1 | 12 | 12 | | | | | | |
| 20 | CTH MC, AT CTH EH INTERSECTION, PLACE RIGHT OF EXISTING J13-1 AND R1-1 SIGN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-3 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | RIGHT |
| 21 | CTH EH, AT CTH MC INTERSECTION, PLACE NEXT TO EXISTING W1-7 SIGN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | LEFT |
| 22 | CTH EH, AT STH 57 INTERSECTION, MODIFY EXISTING J13-2 SIGN AS SHOWN | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | LEFT |
| 23 | STH 57, AT CTH EH INTERSECTION, PLACE ON RIGHT SHOULDER JUST N. OF CTH EH | R 11-3 | 60"X30" | 1 | 12 | 12 | 12 | 24 | | | | 0.8 MILES AHEAD |
| | " | M 4-9R | 30"X24" | 1 | 12 | 12 | | | | | | |
| 24 | CTH EH, AT STH 57 INTERSECTION, MODIFY EXISTING J13-2 SIGN AS SHOWN | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | RIGHT |
| 25 | STH 57, AT CTH EH INTERSECTION, PLACE RIGHT OF EXISTING J3-2 SIGN | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 6-1 | 21"X21" | 1 | 12 | 12 | | | | | | RIGHT |
| 26 | STH 57, S. OF CTH EH, PLACE 750' S. OF CTH EH INTERSECTION | MO 4-8 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 3-1 | 24"X12" | 1 | 12 | 12 | | | | | | |
| | " | M 1-6 | 24"X24" | 1 | 12 | 12 | | | | | | 57 |
| | " | MO 5-1R | 21"X21" | 1 | 12 | 12 | | | | | | |
| 27 | STH 57, S. OF CTH EH, PLACE 750' S. OF CTH EH INTERSECTION | W 20-2A | 48"X48" | 1 | 12 | 12 | | | | | | |
| 28 | STH 57, S. OF 32-57 SPLIT, PLACE S. OF SHEBOYGAN RIVER BRIDGE ON RIGHT SHOULDER | PCMS | | 1 | | | | | 7 | | | FIELD DETERMINED PLACEMENT |
| PAGE SUBTOTALS | | | | 26 | | 300 | 12 | 24 | 7 | | 0 | |
| STRUCTURE B-59-0006 TOTALS | | | | 69 | | 756 | 24 | 48 | 14 | | 4 | |

PLAN SHEET PRODUCED
BY WisDOT - NE REGION

PROJECT NUMBER: 4015-17-60

HWY: STH 57

COUNTY: SHEBOYGAN

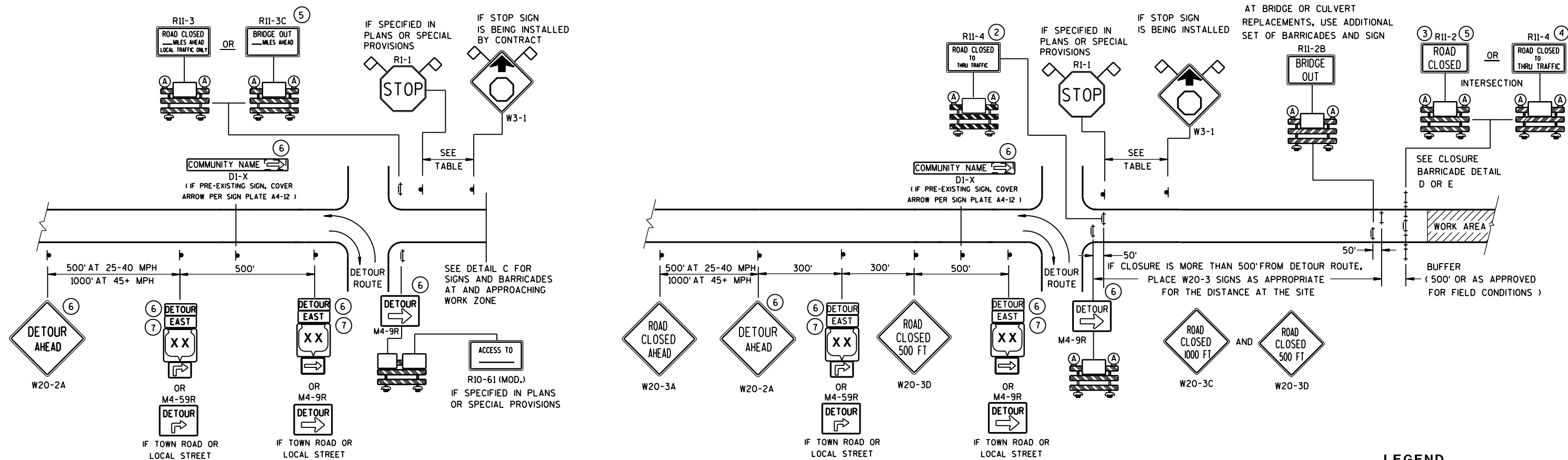
MISCELLANEOUS QUANTITIES FOR STRUCTURE B-59-0006

SHEET

E

Standard Detail Drawing List

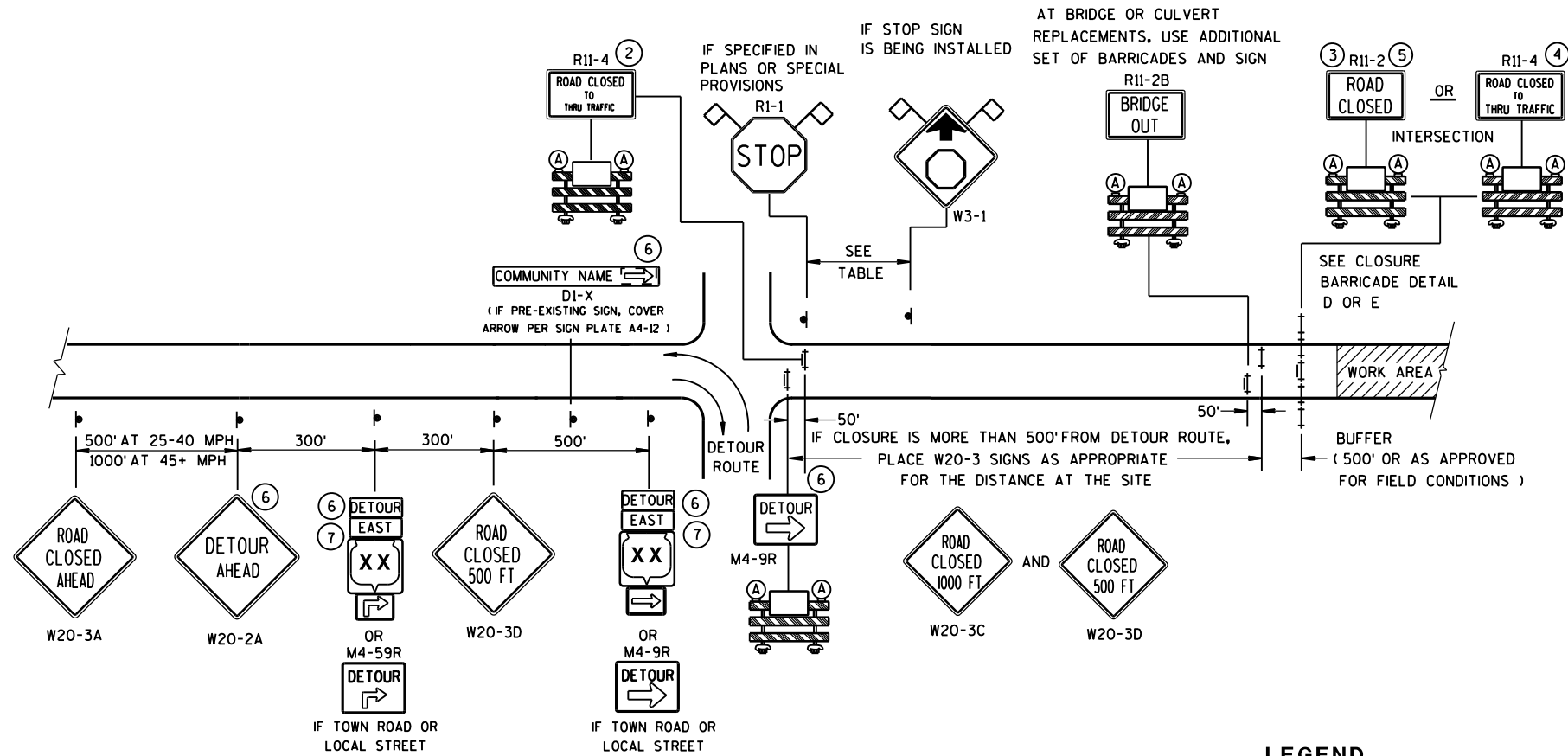
| | |
|-----------|-------------------------------------------------------------------|
| 15C02-06A | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-06B | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-06C | DETOUR SIGNING FOR MAINLINE CLOSURES |
| 15C08-18A | LONGITUDINAL MARKING (MAINLINE) |
| 15C11-07B | CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS |
| 15D38-02A | TEMPORARY TRAFFIC CONTROL SIGN MOUNTING |
| 15D38-02B | ATTACHMENT OF SIGNS TO POSTS |



DETAIL A

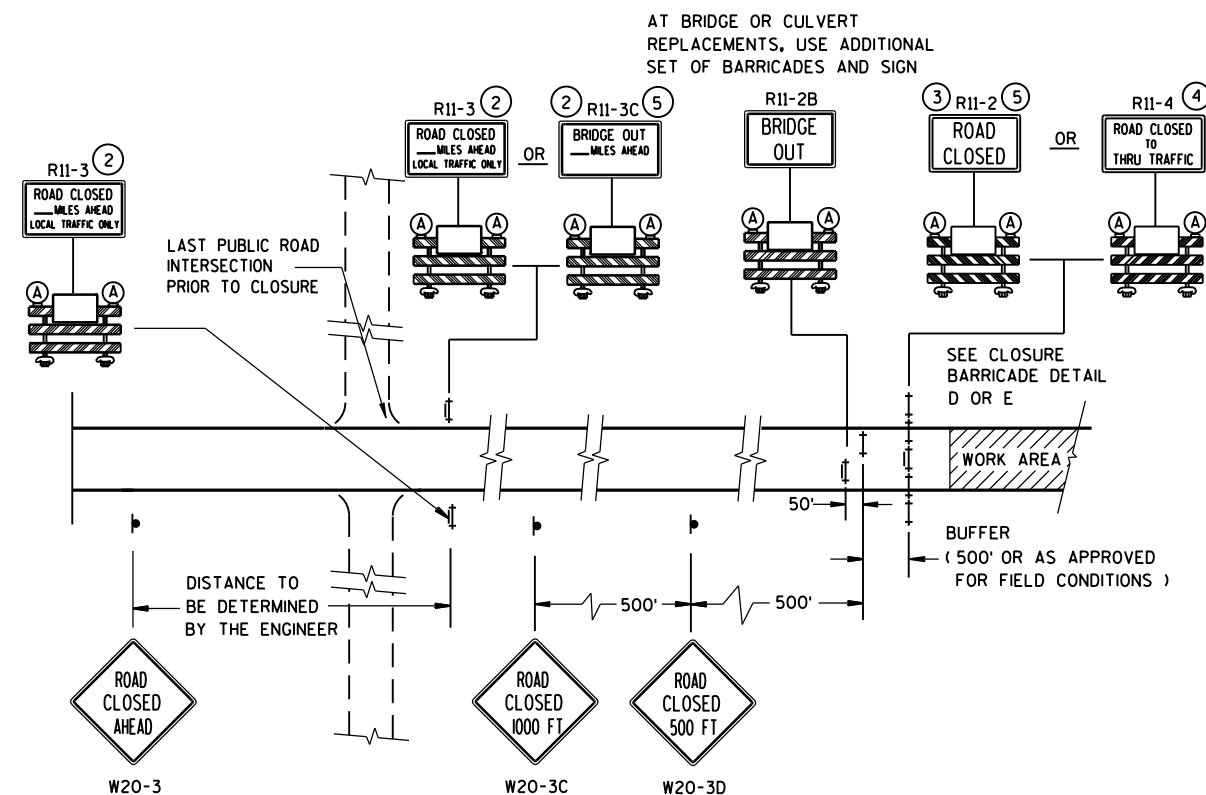
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)
















DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

| SPEED LIMIT (MPH) | "STOP AHEAD" ADVANCE WARNING DISTANCE (FT) |
|-------------------------|-----------------------------------------------------------|
| 25 | 200 |
| 30 | 200 |
| 35 | 350 |
| 40 | 350 |
| 45 | 500 |
| 50 | 550 |
| 55 | 750 |

- # LEGEND
-  SIGN ON PERMANENT SUPPORT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  WORK AREA
-  M4-8
-  M3-X
-  M1-4
- OR
-  M1-5A
- OR
-  M1-6
-  M05-1
- OR
-  M06-1
-  FLAGS, 16" X 16" MIN., (ORANGE)

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES (1) THROUGH (7)

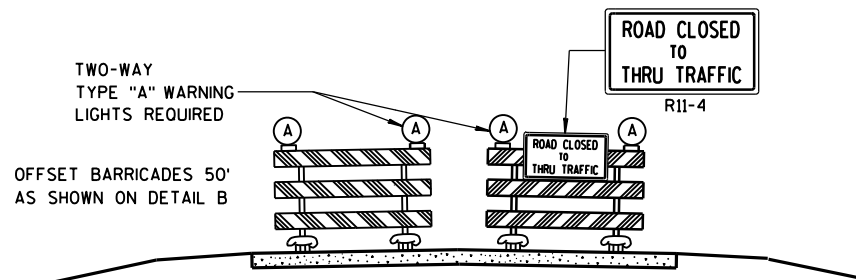
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

| | |
|------------|-----------------------------|
| Sept. 2015 | /S/ Peter Amakobe Atepe |
| DATE | STATEWIDE WORK ZONE TRAFFIC |
| FHWA | SAFETY ENGINEER |



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".

M4-9 SHALL BE 30" X 24".

M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)

M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)

M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)

M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)

D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

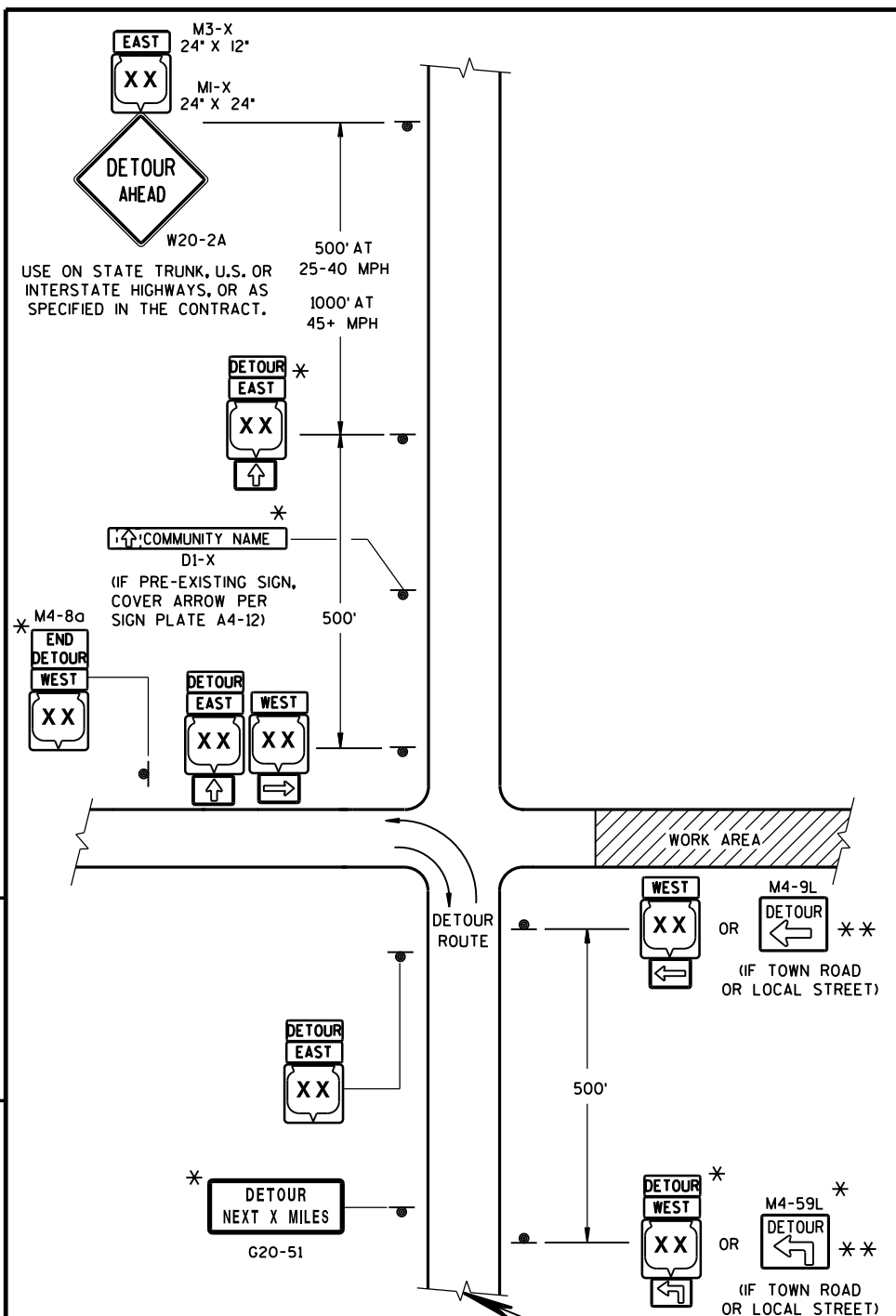
R1-1 SHALL BE 36" X 36".

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 /S/ Peter Amokobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

MATCH POINT

DETAIL F
DETOUR SIGNING

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS, MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

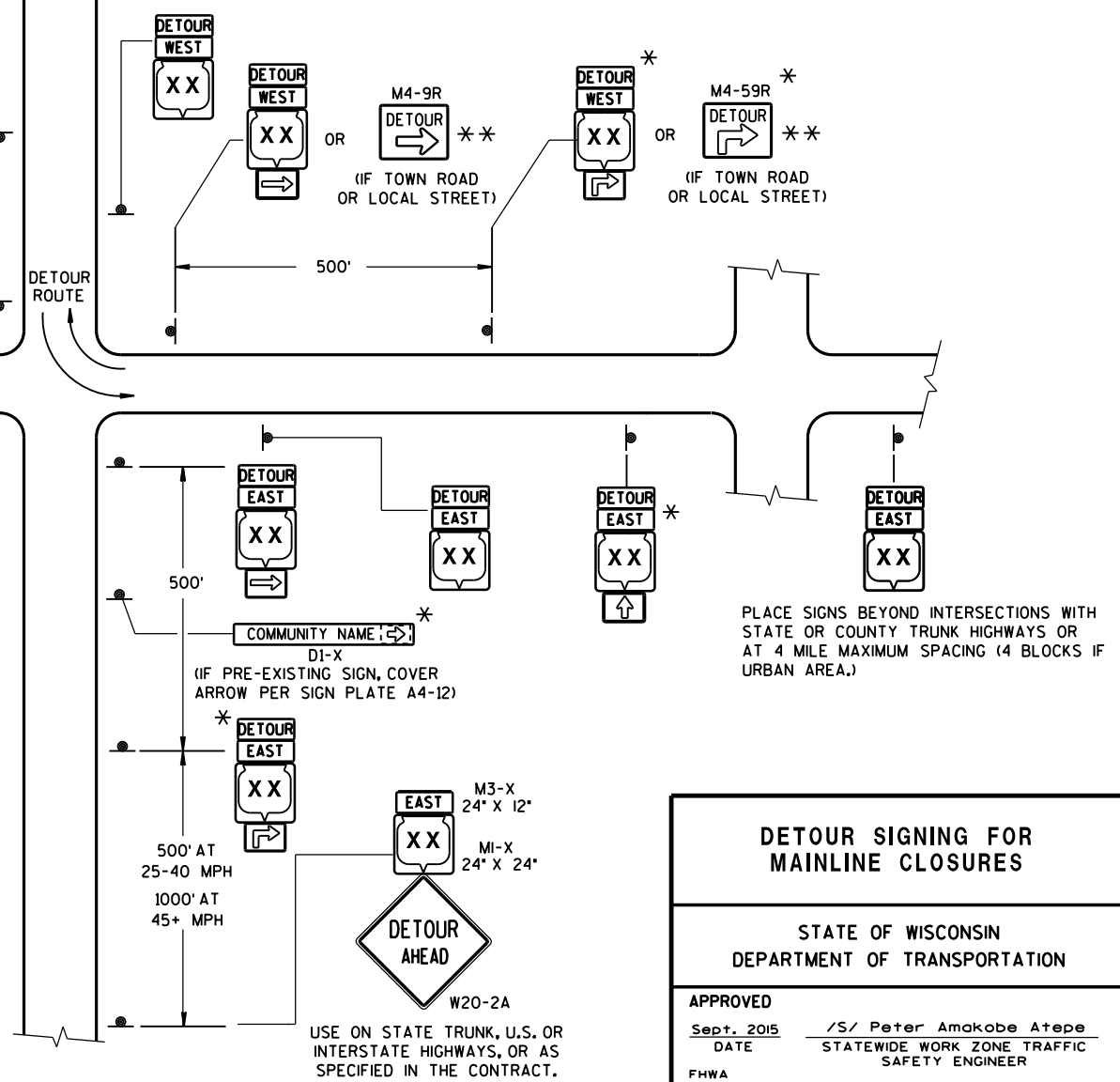
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

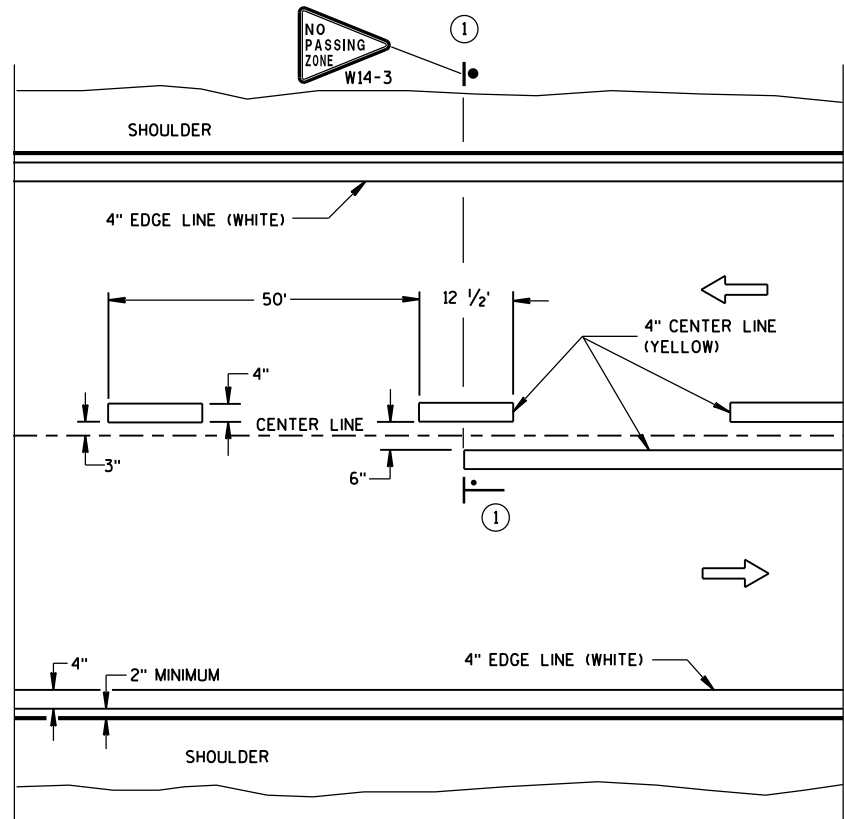
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-9 SHALL BE 30" X 24".
- M4-8a SHALL BE 24" X 18".
- G20-51 SHALL BE 60" X 24".
- W20-2 SHALL BE 48" X 48".
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

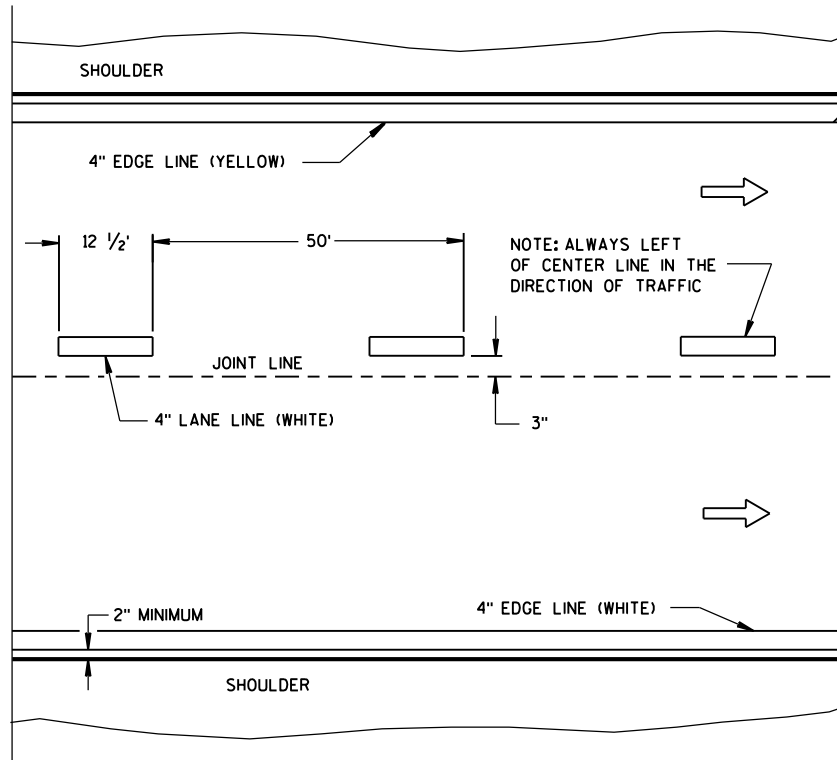
** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



| DETOUR SIGNING FOR MAINLINE CLOSURES | |
|----------------------------------------------------|------------------------------------------------------------------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED | /S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER |
| Sept. 2015 DATE | |
| FHWA | |

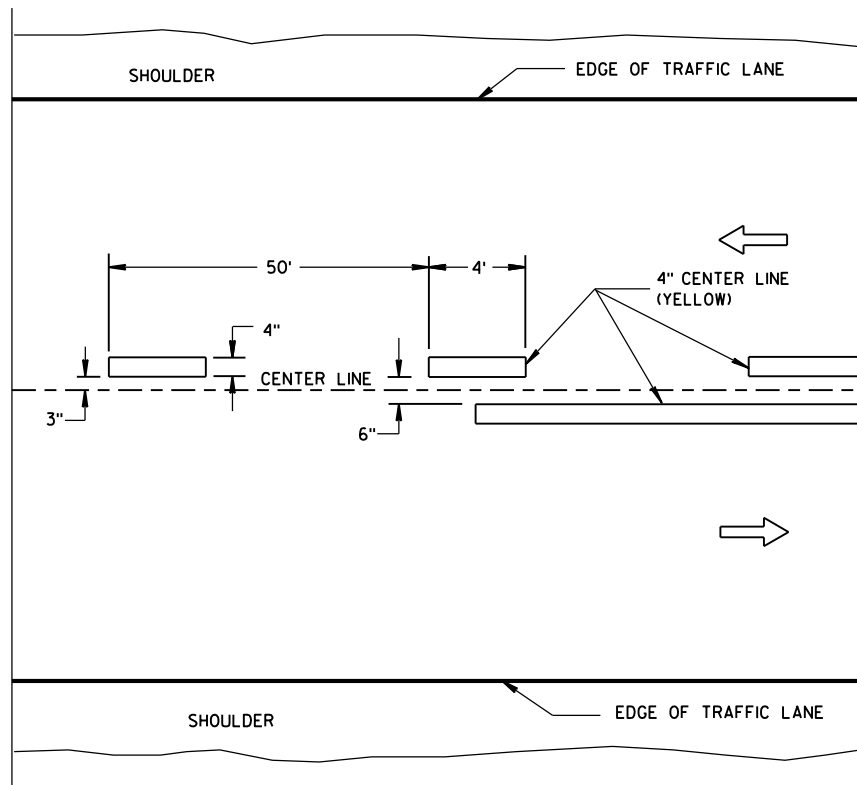


TWO WAY TRAFFIC

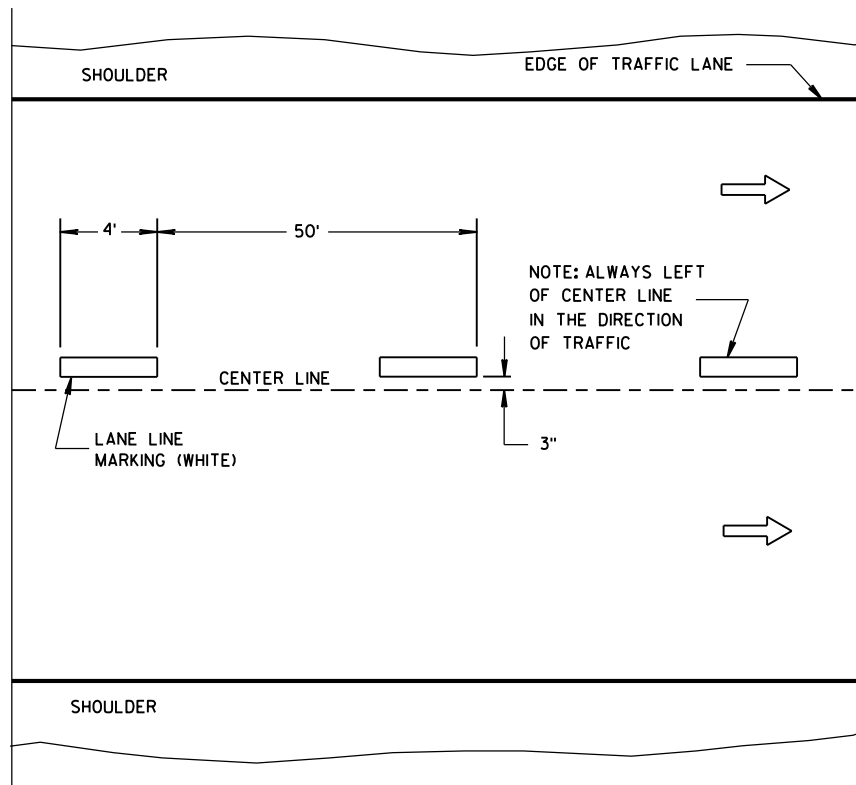


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

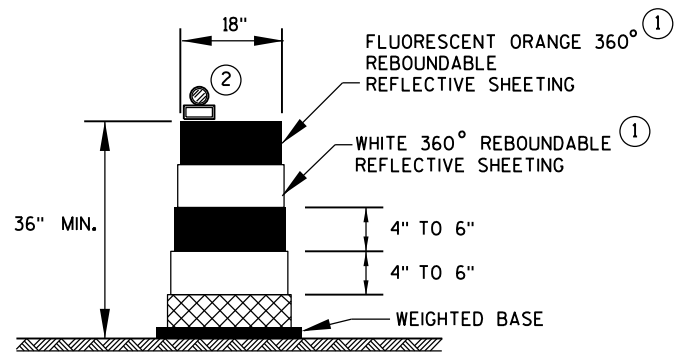
LEGEND

- "T" MARKING
- POST MOUNTED SIGN

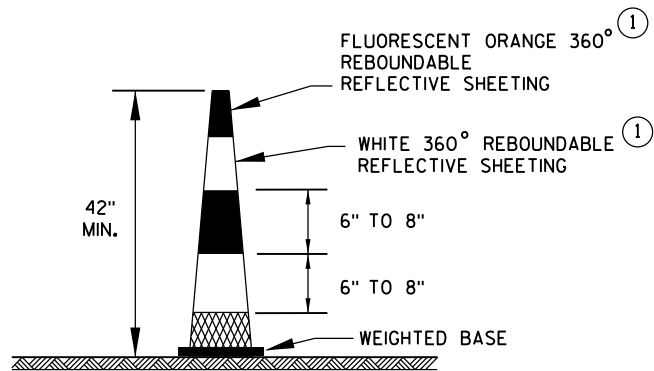
LONGITUDINAL MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

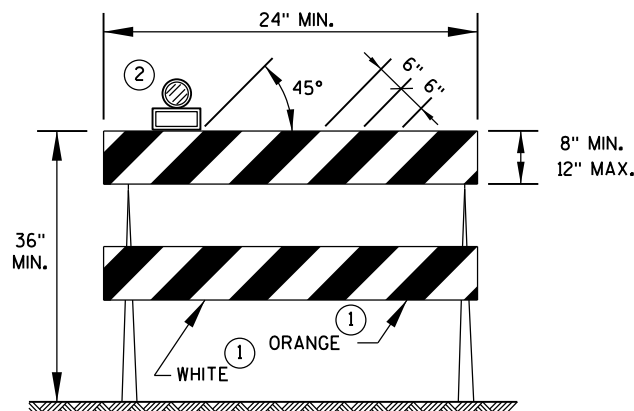


DRUM



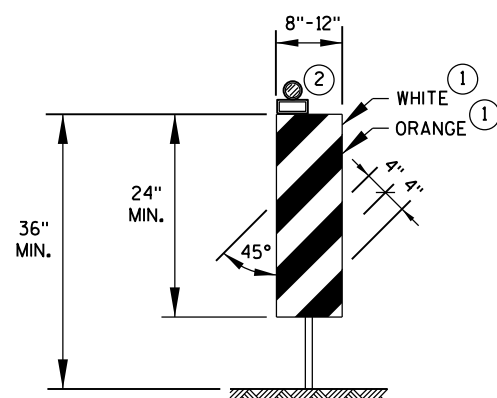
42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS



TYPE 2 BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

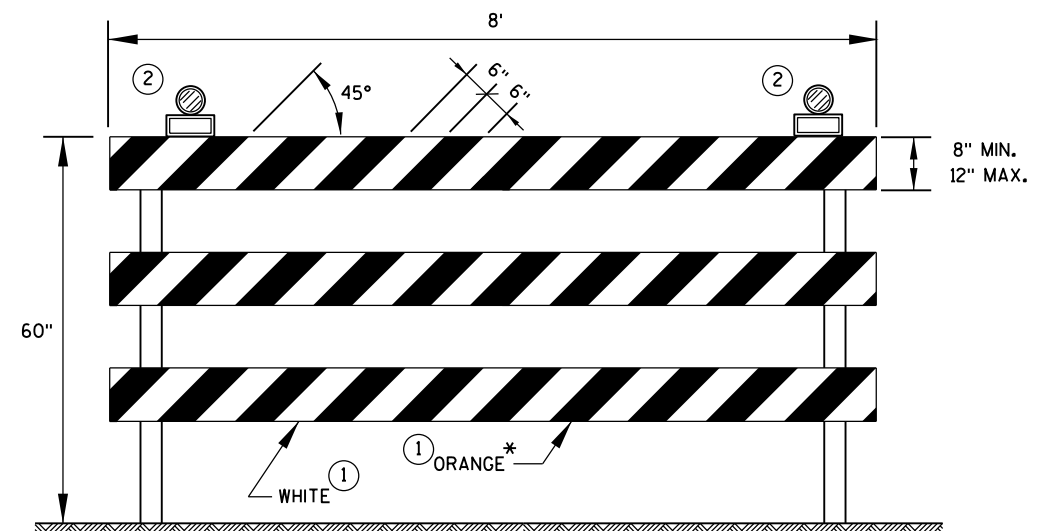


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE 3 BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

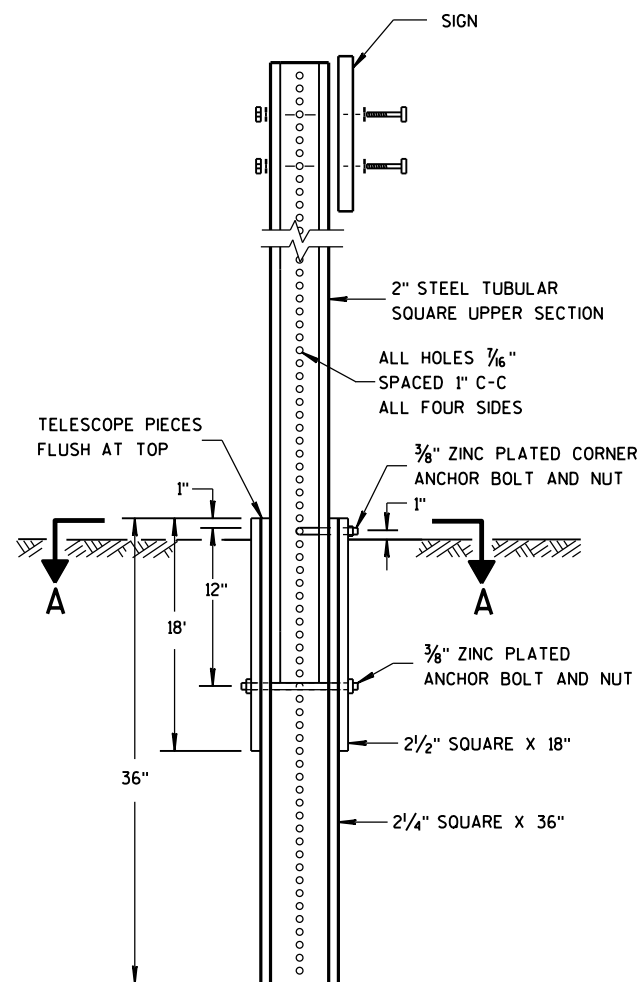
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017
DATE

FHWA

/S/ Andrew Heidtke
WORK ZONE ENGINEER



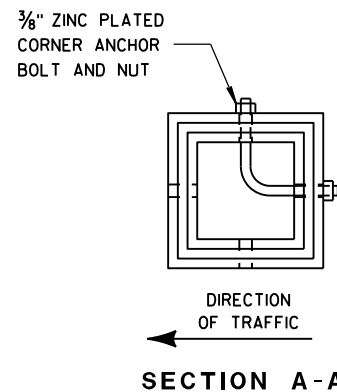
DETAIL OF TUBULAR
STEEL SIGN POST

TUBULAR STEEL POSTS

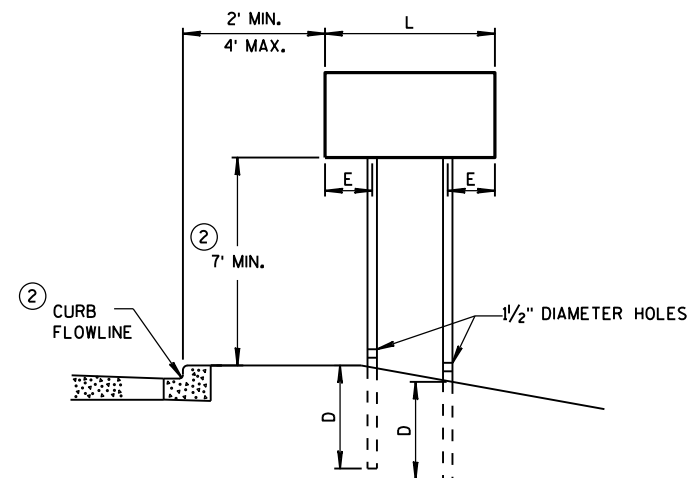
| AREA OF SIGN INSTALLATION (SQ. FT.) | NUMBER OF REQUIRED TUBULAR STEEL POSTS |
|------------------------------------------------|----------------------------------------------|
| 9 OR LESS | 1 |
| GREATER THAN 9 LESS THAN OR EQUAL TO 18 | 2 |
| GREATER THAN 18 LESS THAN OR EQUAL TO 27 | 3 |

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL
BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED
ON TUBULAR STEEL POSTS.



SECTION A-A

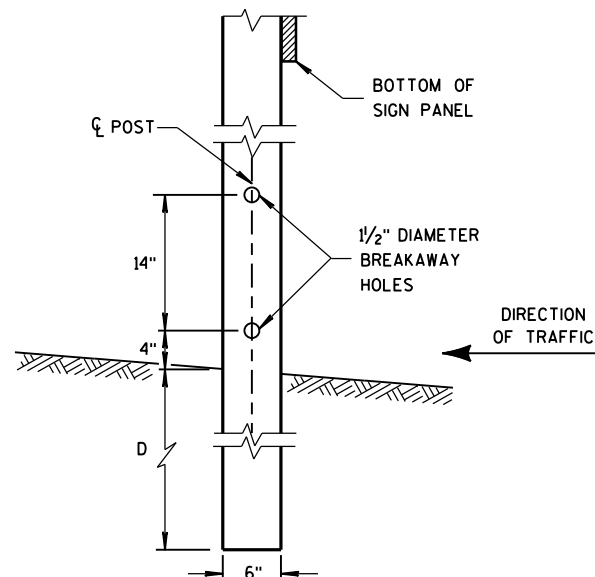


URBAN AREA

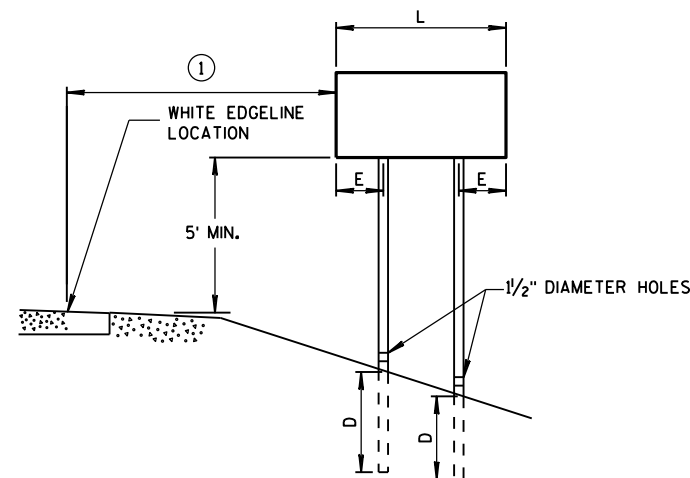
POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST
EMBEDMENT DEPTH

| AREA OF SIGN INSTALLATION (SQ. FT.) | D (MIN) |
|-------------------------------------------|------------|
| 20 OR LESS | 4' |
| GREATER THAN 20 | 5' |



4 "x6 " WOOD POST
MODIFICATION



RURAL AREA

4 " X 6 " WOOD POST

| POST SPACING REQUIREMENTS | | NUMBER OF WOOD POSTS REQUIRED |
|-----------------------------------------|-----|-------------------------------------|
| L | E | |
| 48" OR LESS AND LESS THAN 20 SQ. FT. | - | 1 |
| LESS THAN 60" | 12" | 2 |
| 60" TO 120" | L/5 | 2 |
| GREATER THAN 120" LESS THAN 168" | 12" | 3 |
| 168" AND GREATER | 12" | 4 |

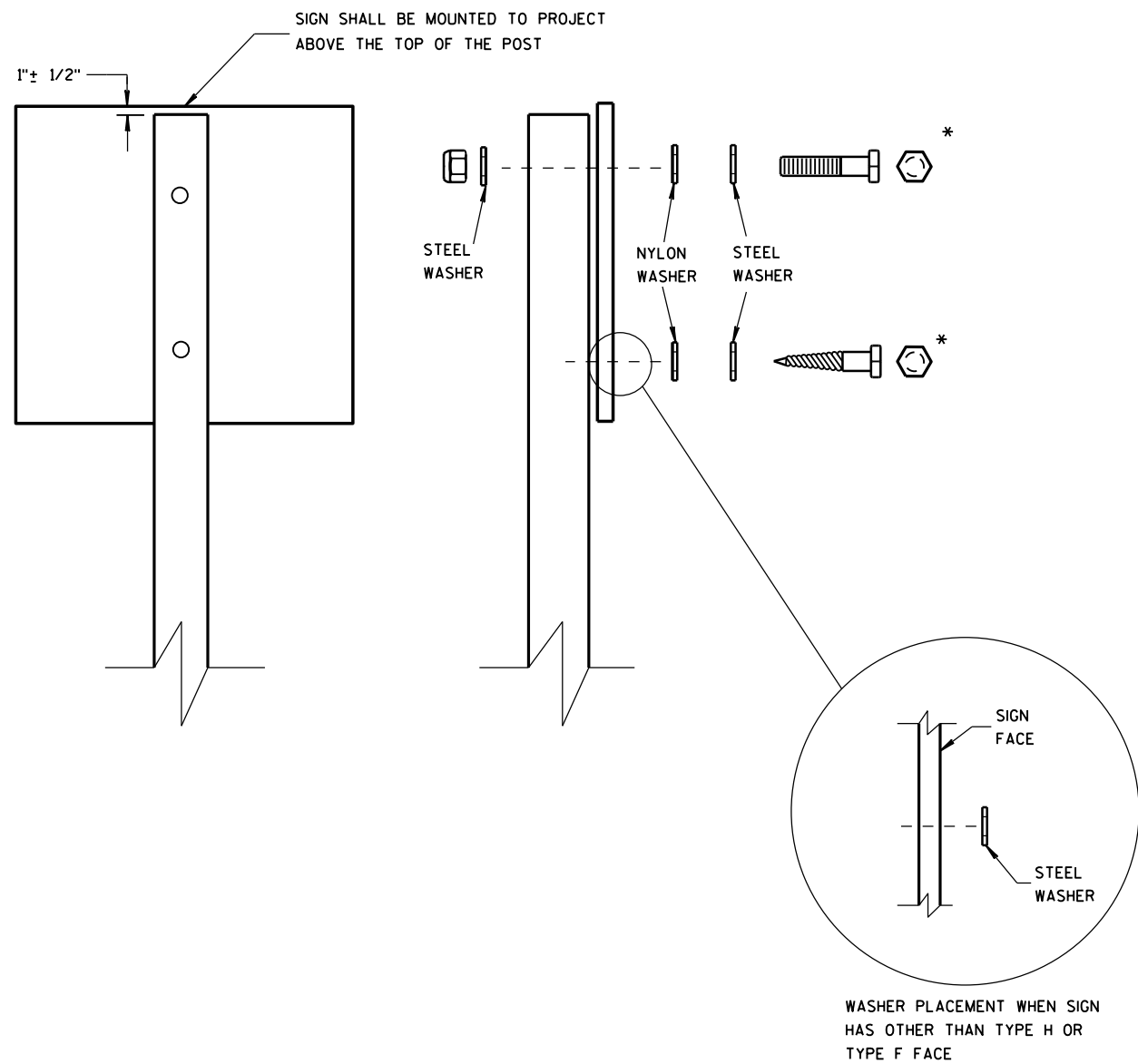
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL
SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" x 3"
 - MACHINE BOLTS - 5/16" x 6-1/2" OR 7" LENGTH W/ NUTS

- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" x 3-1/4" LENGTH W/ NUTS
 - RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

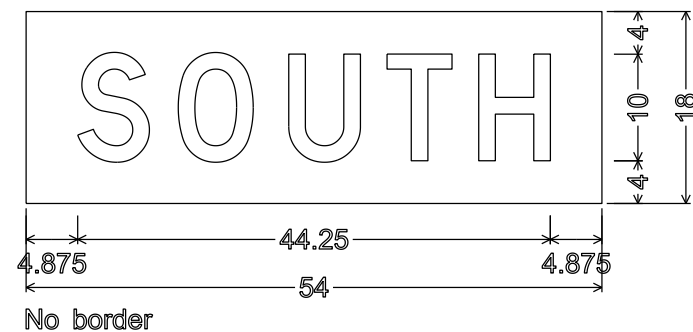
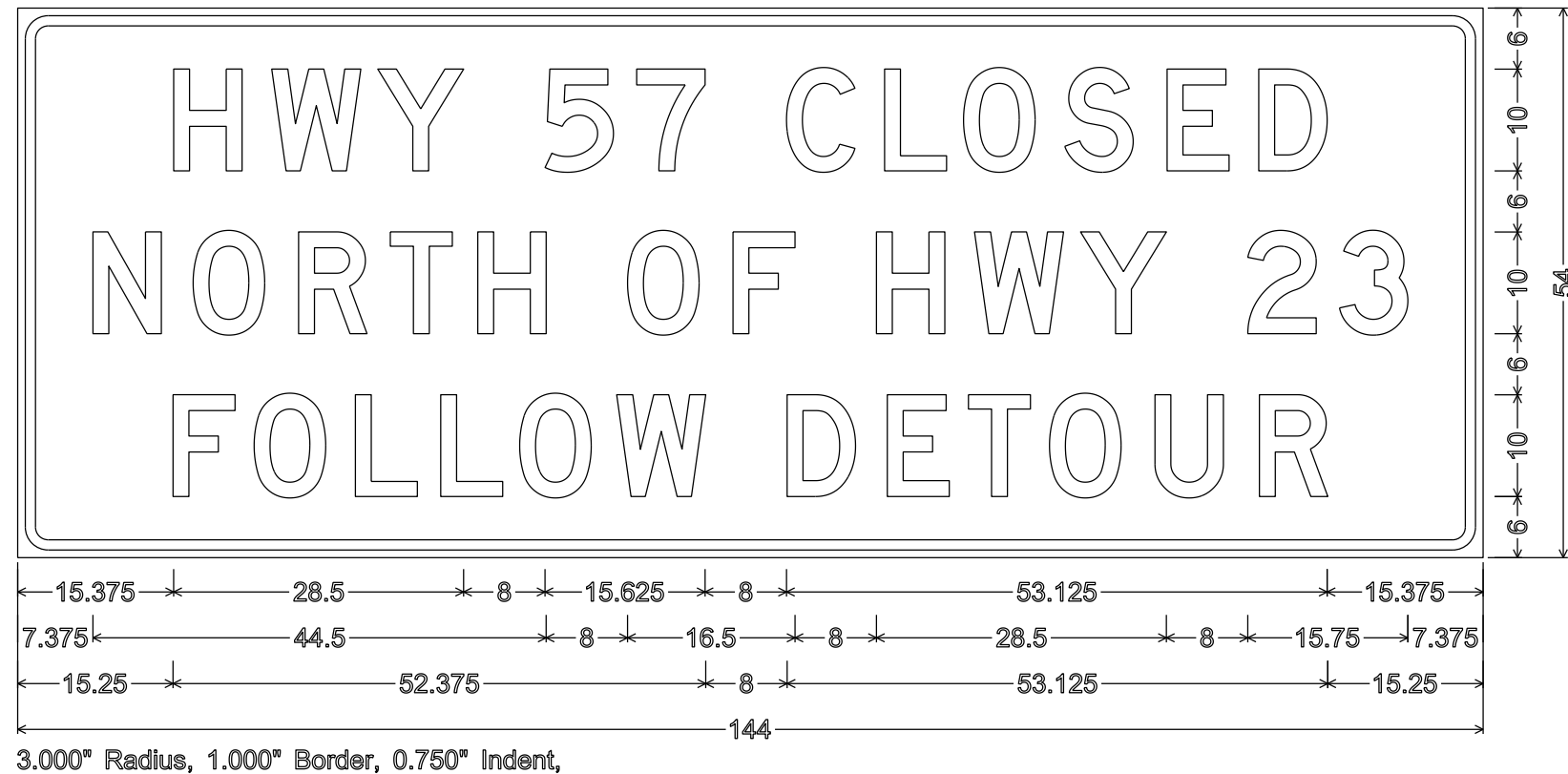
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL
 - 1-1/4" O.D. x 3/8" I.D. x .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

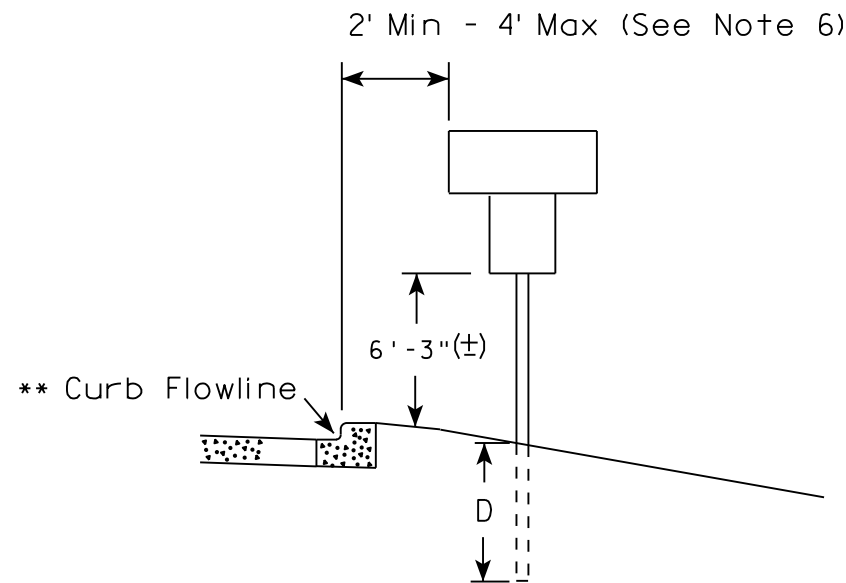
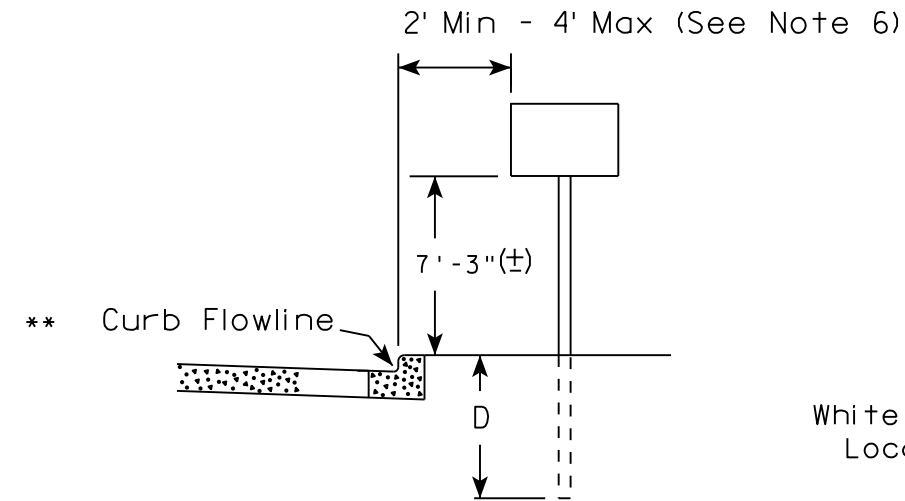
| | |
|----------------------------------------------------|------------------------------------------|
| ATTACHMENT OF SIGNS TO POSTS | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED June 2017 DATE | /S/ Andrew Heidtke WORK ZONE ENGINEER |
| FHWA | |

NOTES

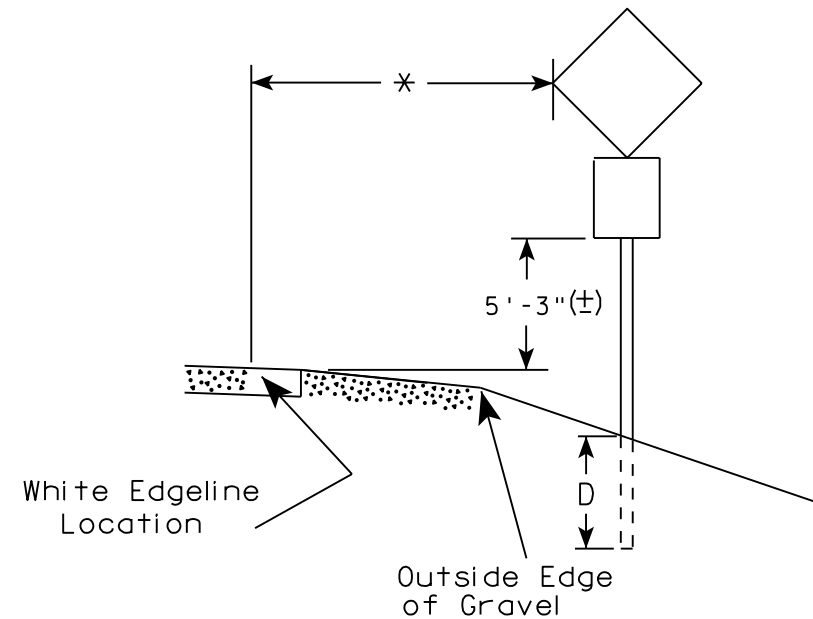
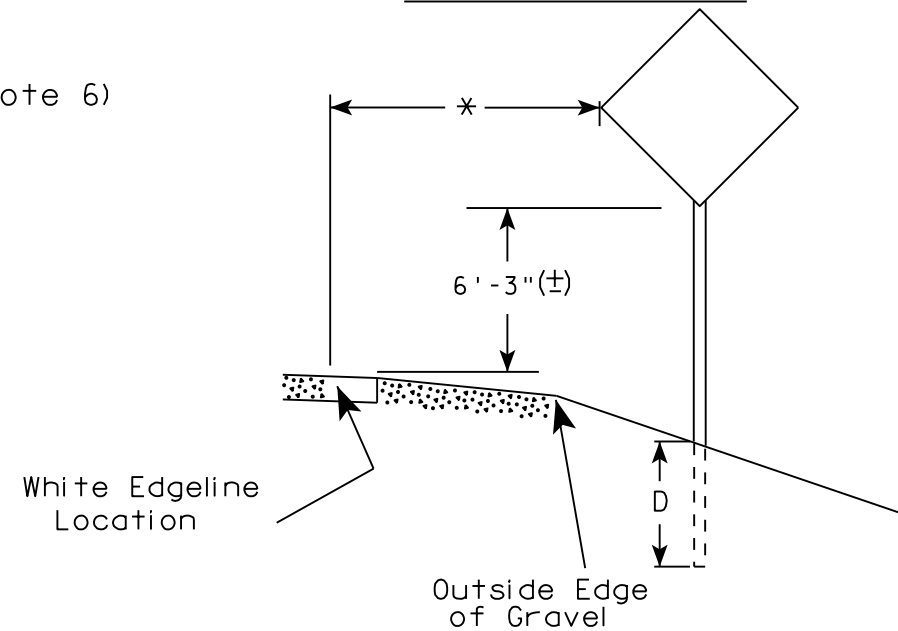
1. Signs are Type II- Type F Reflective
2. Color:
 - Background - Orange
 - Message - Black
3. Message Series - D



URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|------------------------------------------|--------------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. J-Assemblies are considered to be one sign for mounting height.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

✱✱ The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

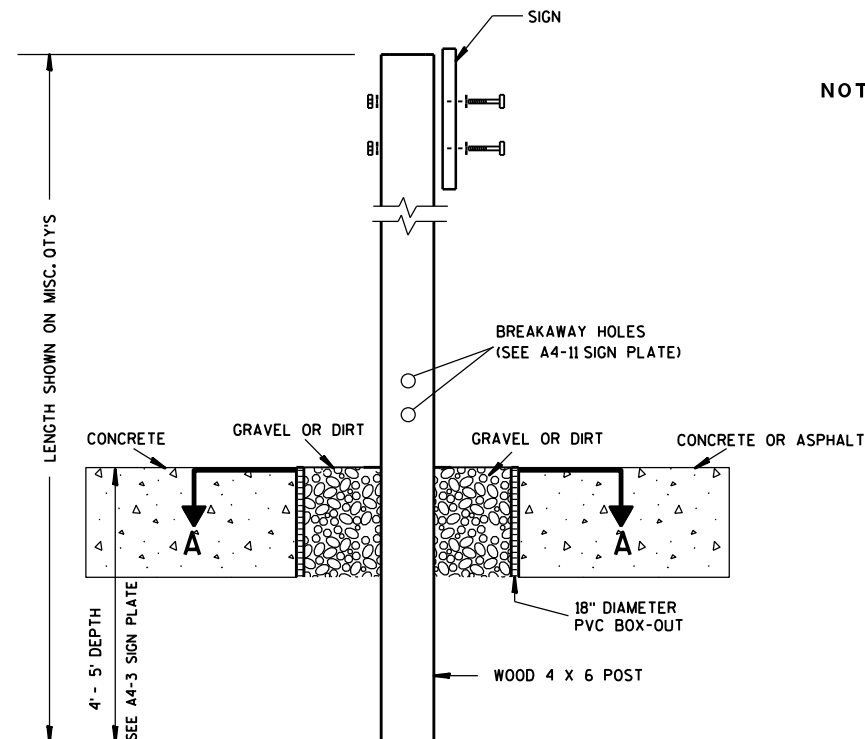
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

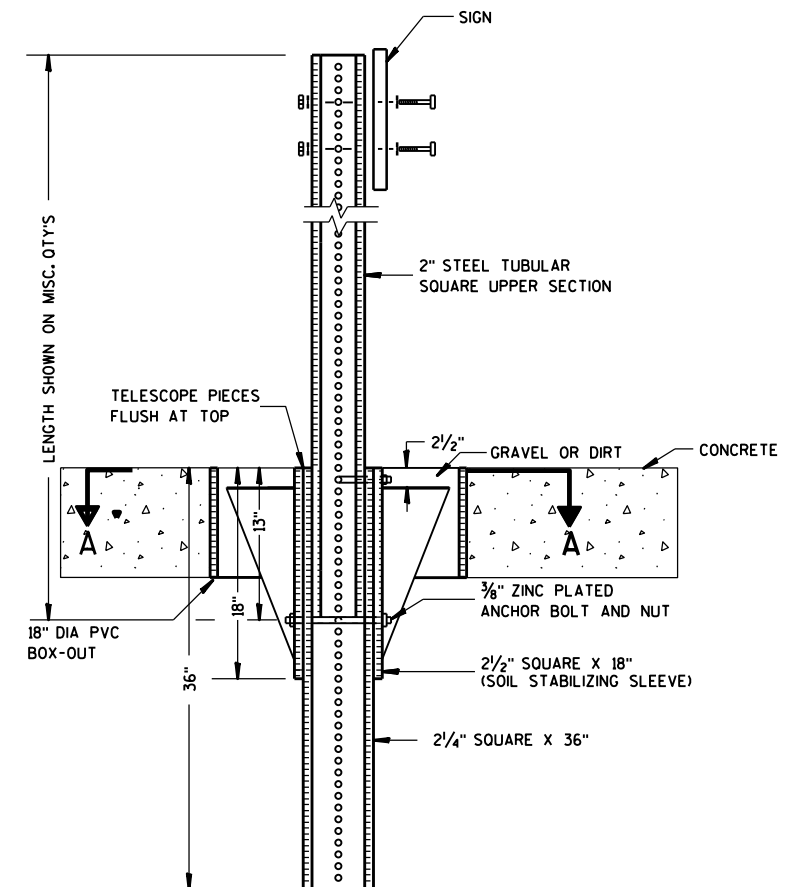
DATE 8/21/17 PLATE NO. A4-3.21



ELEVATION VIEW

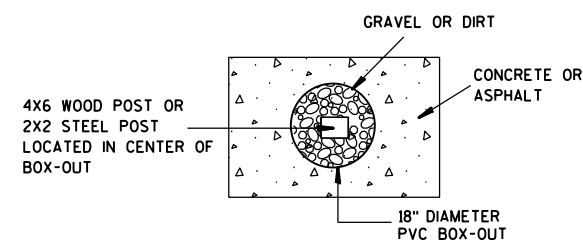
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

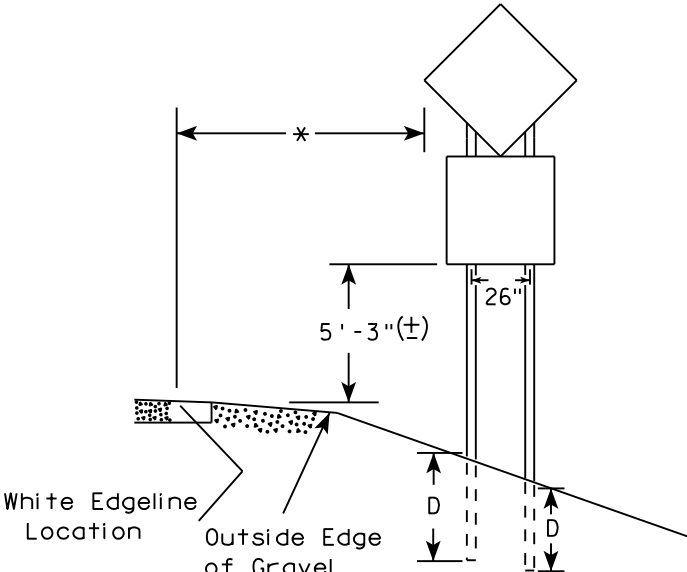
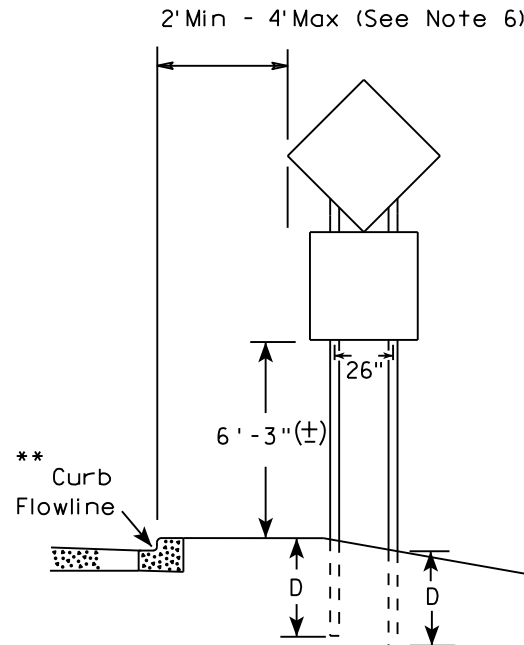
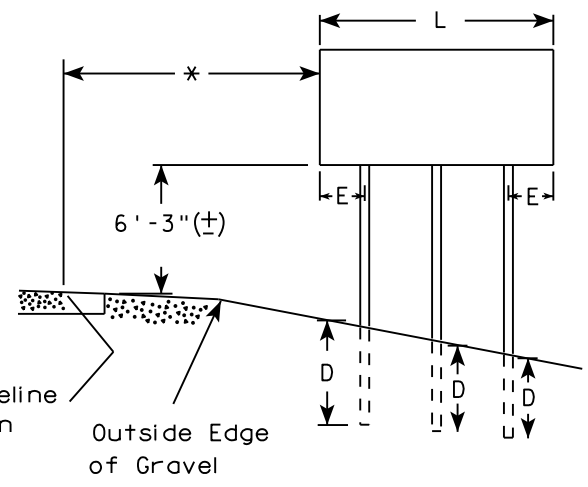
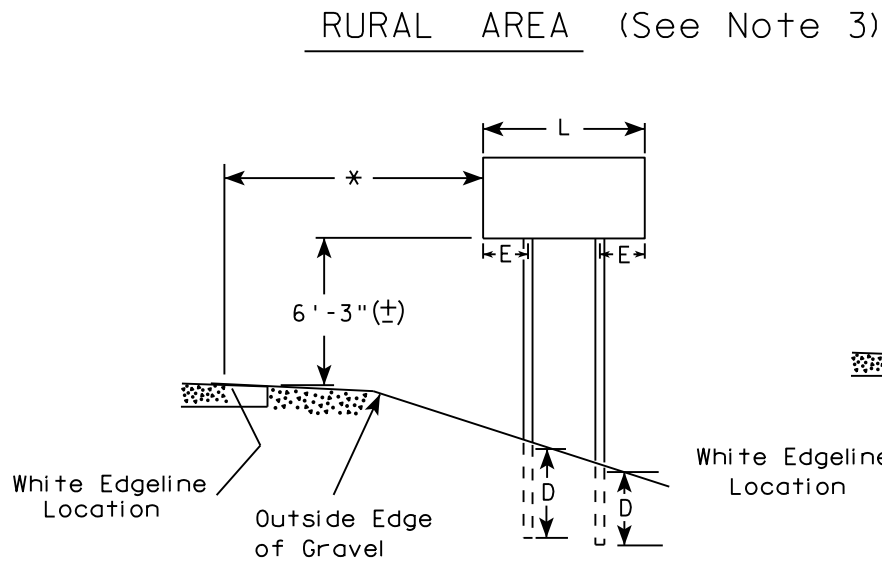
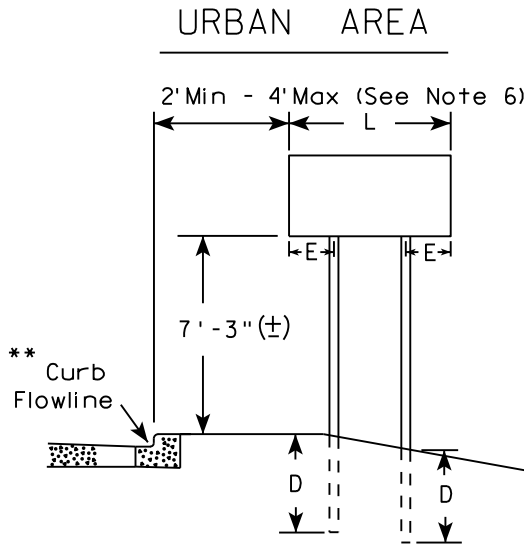
FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1



| SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED) | |
|-------------------------------------------------------|-----|
| L | E |
| Greater than 48" Less than 60" | 12" |
| 60" to 108" | L/5 |

| SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED) | |
|---------------------------------------------------------|-----|
| L | E |
| Greater than 108" to 144" | 12" |

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|---------------------------------------------|--------------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

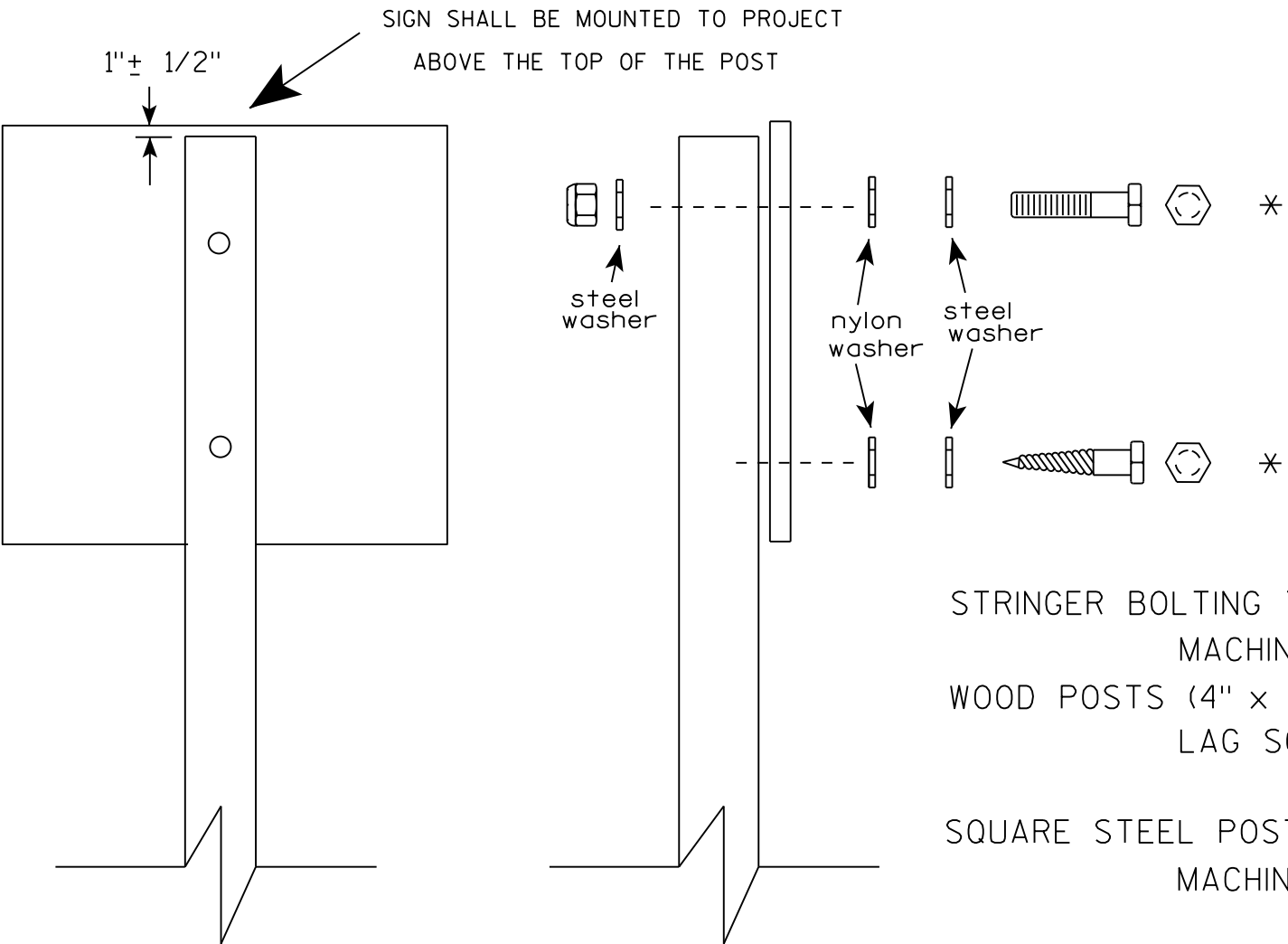
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15

- GENERAL NOTES
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. J-Assemblies are considered to be one sign for mounting height.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- *** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

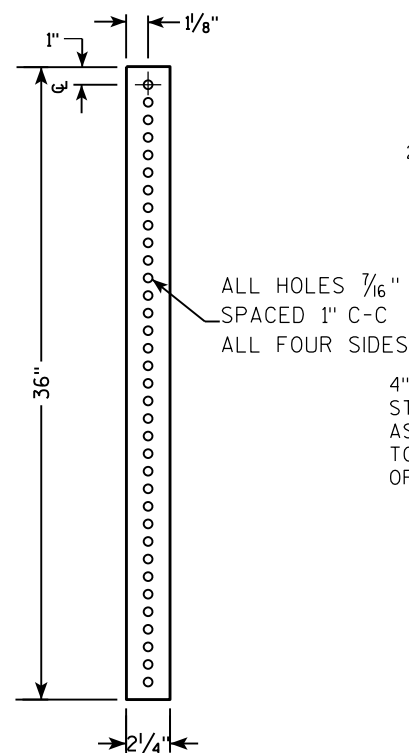
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
 - 3/8" X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - 3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - 9/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 - 1-1/4" O.D. X 3/8" I.D. X .080 NYLON

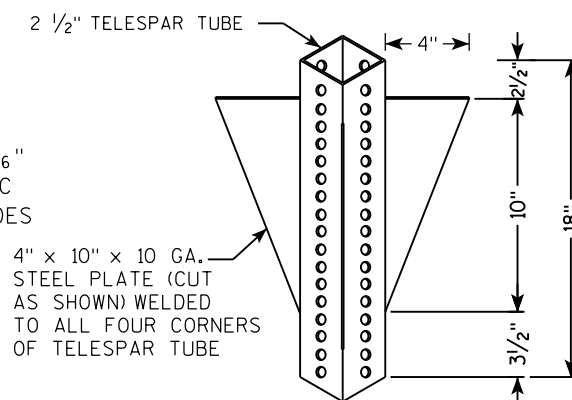
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

| | |
|----------------------------------|-------------------------------------------------------|
| ATTACHMENT OF SIGNS TO POSTS | |
| WISCONSIN DEPT OF TRANSPORTATION | |
| APPROVED | <i>Matthew R. Rauch</i> For State Traffic Engineer |
| DATE 8/11/16 | PLATE NO. A4-8.8 |

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



TECHNICAL DRAWING OF A VERTICAL SIGNPOST ASSEMBLY.

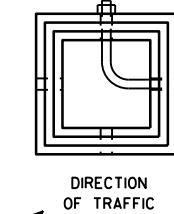
Labels and Dimensions:

- 18" DIA SCHEDULE 40 PVC BOX-OUT**: The base container for the assembly.
- 36"**: Total height of the PVC box-out.
- 18"**: Height of the gravel/dirt section at the base.
- 13"**: Height of the upper section of the PVC box-out.
- 2 1/2" GRAVEL OR DIRT**: The base layer within the box-out.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: The sleeve supporting the upper section.
- 2 1/4" SQUARE X 36"**: The main vertical support structure.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The upper part of the main support.
- ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES**: Specification for the holes in the steel tubular section.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Hardware securing the sleeve.
- 3/16" ZINC PLATED ANCHOR BOLT AND NUT**: Hardware securing the base.
- SIGN**: The sign plate at the top.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to the sign plate for hardware details.
- TELESCOPE PIECES FLUSH AT TOP**: Instruction for the top of the assembly.

TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY:

- TELESCOPE PIECES FLUSH AT TOP**: Indicated by a dimension line on the left.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The main vertical support.
- ALL HOLES $\frac{7}{16}$ " SPACED 1" C-C ALL FOUR SIDES**: Specification for the perforations in the tubular section.
- SIGN**: Attached to the top of the post.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to a separate drawing for hardware.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Hardware used to secure the post to the base.
- 1"**: Dimension for the offset of the anchor bolt from the post face.
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT**: Hardware used to secure the base plate to the ground.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: The base plate.
- 2 1/4" SQUARE X 36"**: The main vertical post.
- Dimensions**:
 - 36" (Total height of the post section)
 - 18" (Height of the upper section)
 - 12" (Height of the lower section)
- Arrows A**: Indicate downward forces or weights applied to the post.

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT



SECTION A-A

| Area of Sign Installation (Sq. Ft.) | Number of Required Posts |
|------------------------------------------|--------------------------|
| 9 or less | 1 |
| Greater than 9 less than or equal to 18 | 2 |
| Greater than 18 less than or equal to 27 | 3 |

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

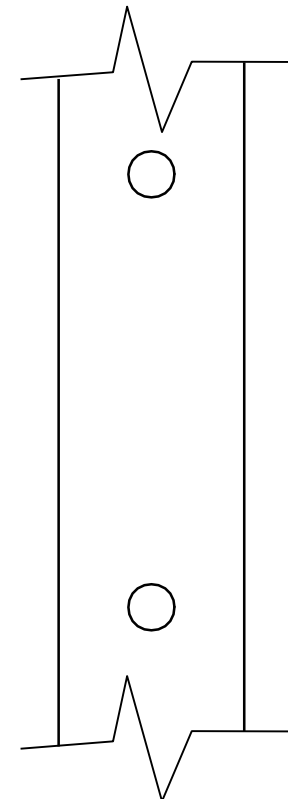
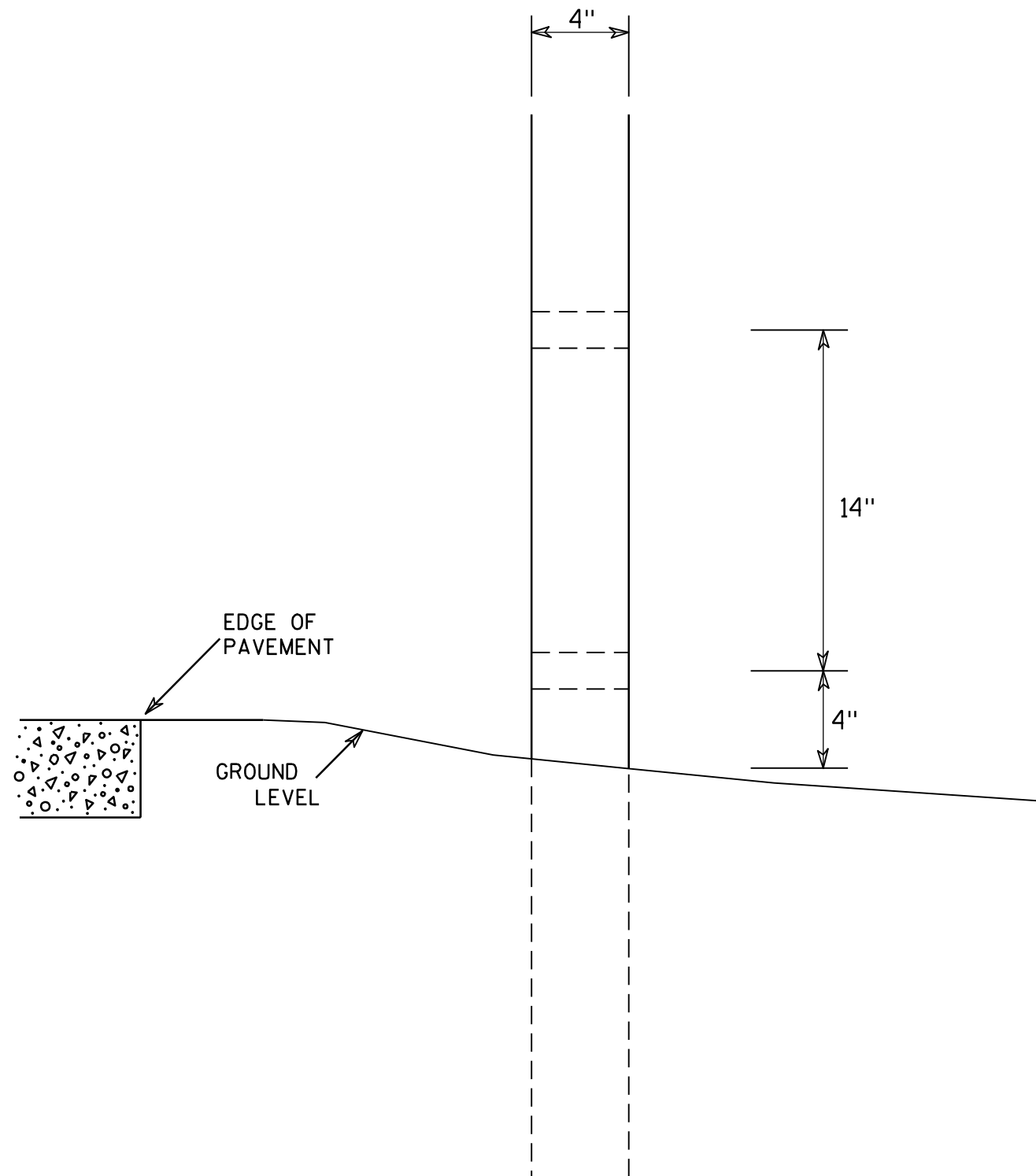
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1 1/2" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

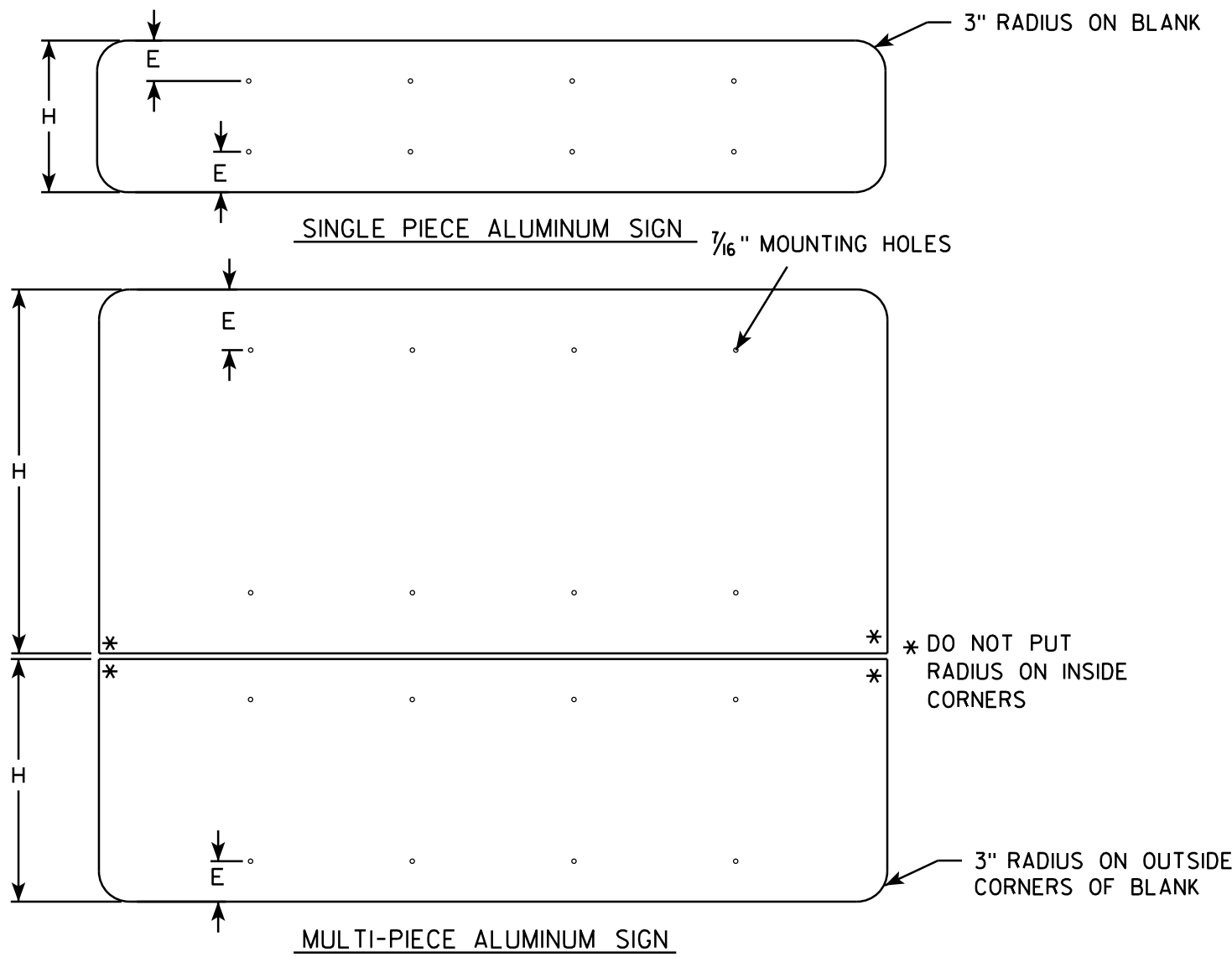
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

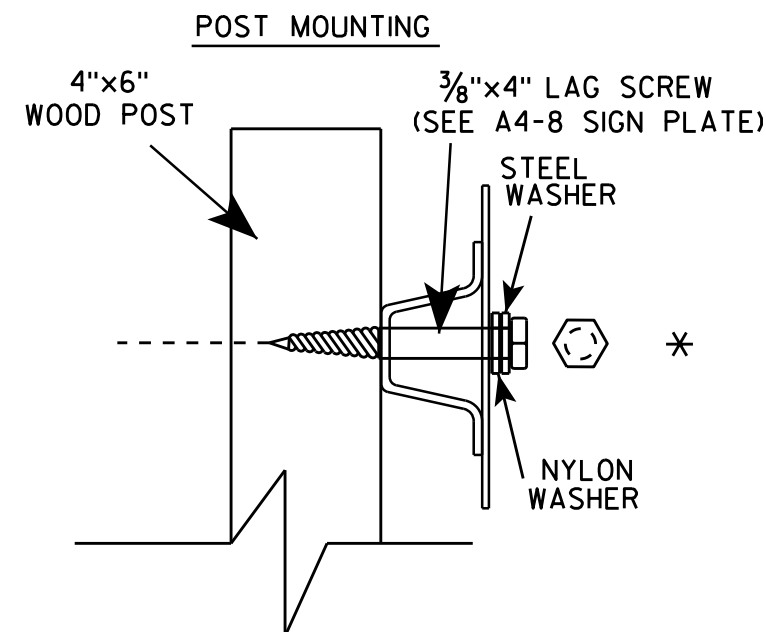
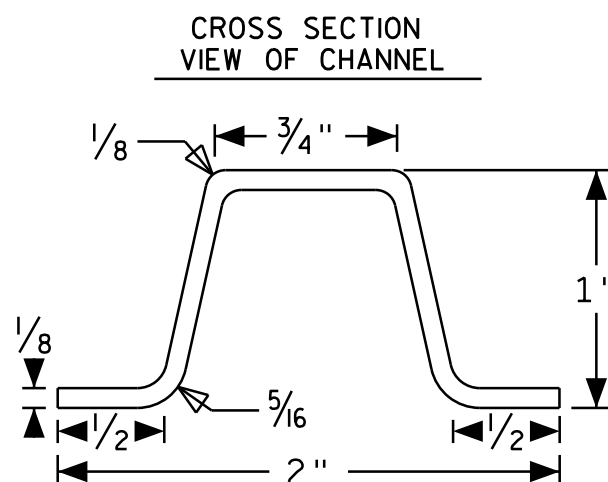
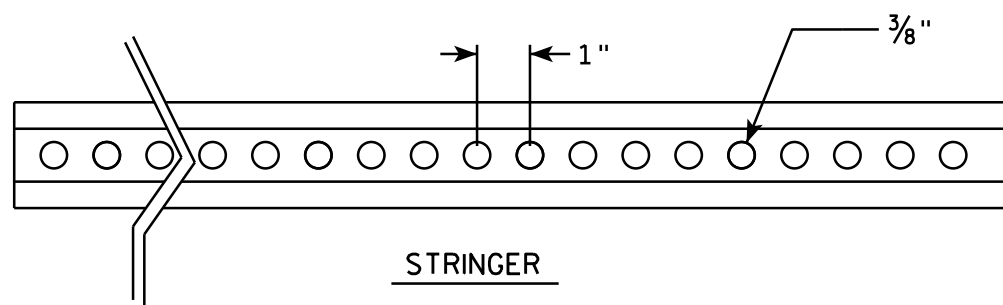
E



GENERAL NOTES

- ALL SIGNS OVER 60" IN WIDTH SHALL HAVE A 3" RADIUS ON THE OUTSIDE CORNERS OF THE ALUMINUM BLANK.
- MOUNTING HOLES SHALL BE 7/16" DIAMETER.
- SEE CHART FOR HOLE SPACING REQUIREMENTS
- FOR SIGN PANELS WITH DIMENSION (H) 36" AND OVER, DIMENSION E SHALL BE 6"
- FOR SIGN PANELS WITH DIMENSION (H) UNDER 36", DIMENSION E SHALL BE 4"
- SIGN STRINGER MATERIAL SHALL CONSIST OF STEEL CHANNEL POST SECTIONS, WEIGHING 1.12 LBS/FT IN ACCORDANCE WITH SECTION 633.2.1 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- SEE SIGN PLATE A4-8 FOR SIGN STRINGER BOLTING REQUIREMENTS.

| SIGN WIDTH | STRINGER WIDTH | POSTS | HOLE SPACING | MOUNTING HOLES | | | |
|------------|----------------|-------|--------------|----------------|---------|---------|-------------------|
| 78" | 72" | 2 | 16" | 15" | 31" | 47" | 63" |
| 84" | 72" | 2 | 17" | 16 1/2" | 33 1/2" | 50 1/2" | 67 1/2" |
| 90" | 72" | 2 | 18" | 18" | 36" | 54" | 72" |
| 96" | 90" | 2 | 19" | 19 1/2" | 38 1/2" | 57 1/2" | 76 1/2" |
| 102" | 90" | 2 | 20" | 21" | 41" | 61" | 81" |
| 108" | 90" | 2 | 21" | 22 1/2" | 43 1/2" | 64 1/2" | 85 1/2" |
| 114" | 108" | 3 | 15" | 12" | 27" | 42" | 57" 72" 87" 102" |
| 120" | 108" | 3 | 16" | 12" | 28" | 44" | 60" 76" 92" 108" |
| 126" | 108" | 3 | 17" | 12" | 29" | 46" | 63" 80" 97" 114" |
| 132" | 126" | 3 | 18" | 12" | 30" | 48" | 66" 84" 102" 120" |
| 138" | 126" | 3 | 19" | 12" | 31" | 50" | 69" 88" 107" 126" |
| 144" | 126" | 3 | 20" | 12" | 32" | 52" | 72" 92" 112" 132" |



SIGN STRINGER
MOUNTING REQUIREMENTS

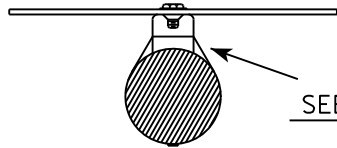
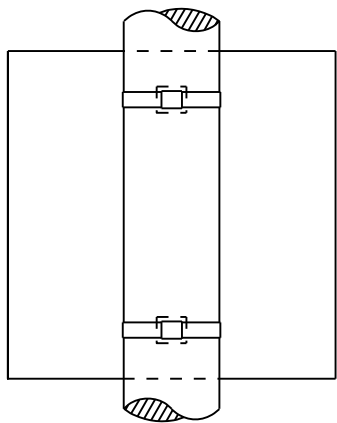
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/26/16 PLATE NO. A4-18.1

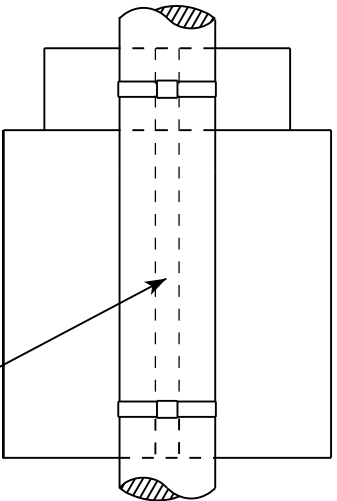
BANDING

SINGLE SIGN

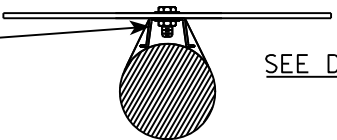


SEE DETAIL A

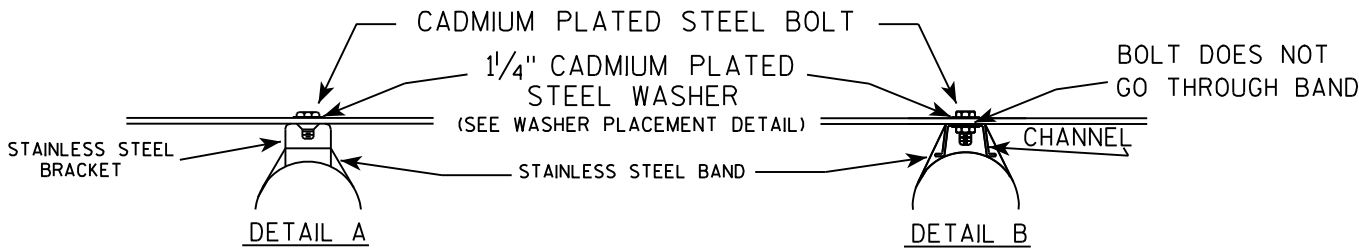
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



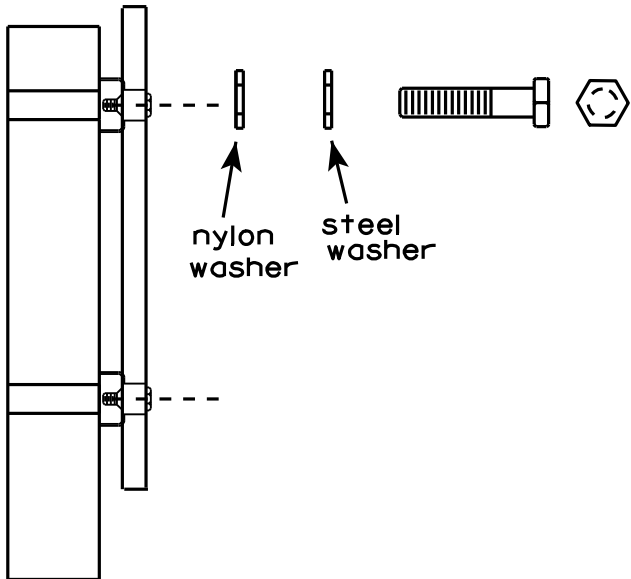
SEE DETAIL B



GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.

WASHER PLACEMENT



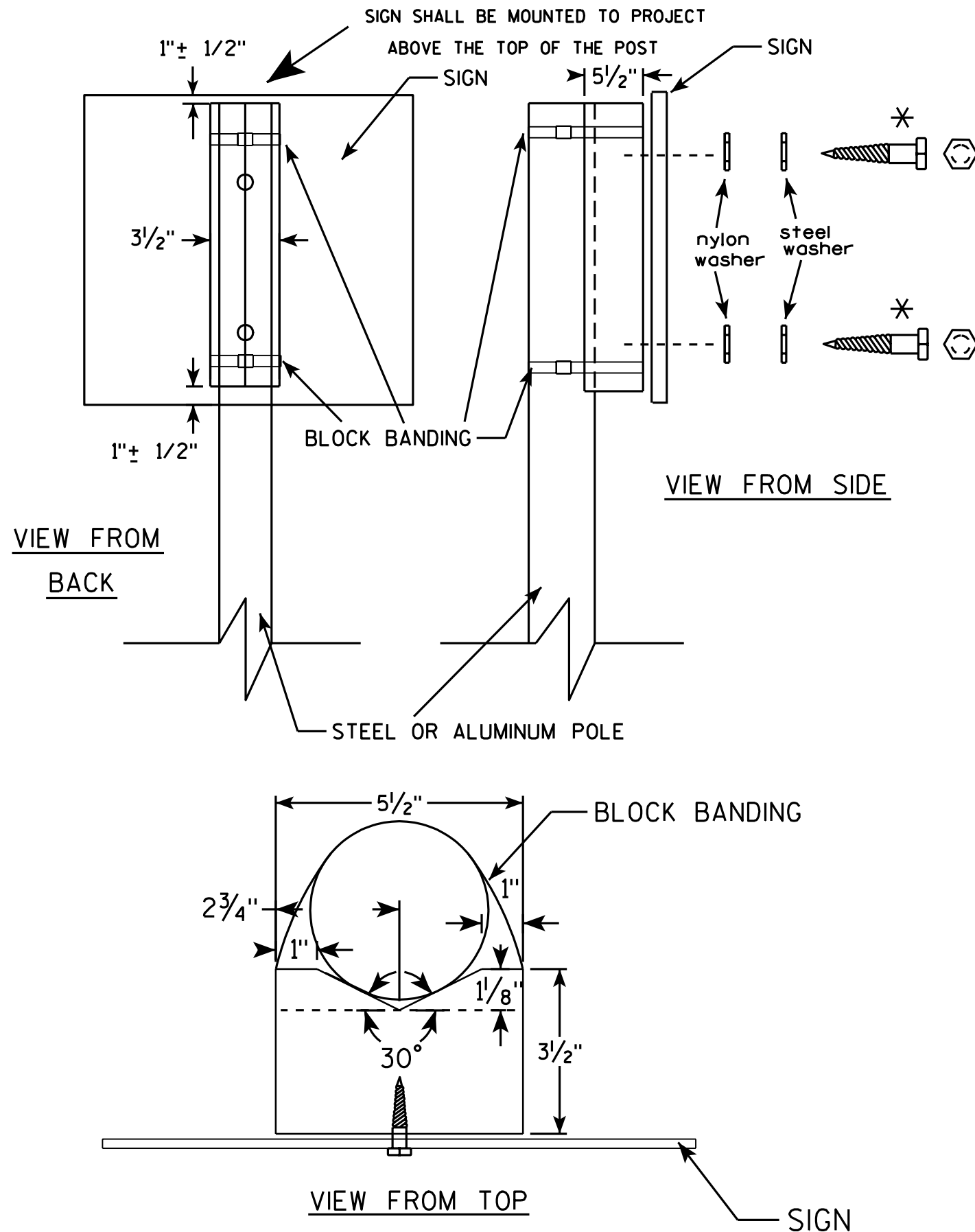
WASHERS (ALL POSTS) -
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/16/13 PLATE NO. A5-9.3



GENERAL NOTES

1. WOOD 4"x6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D, or
 - b. Cadmium plated in accordance with ASTM Designation : B 766 TYPE 3, Class 12, or
 - c. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

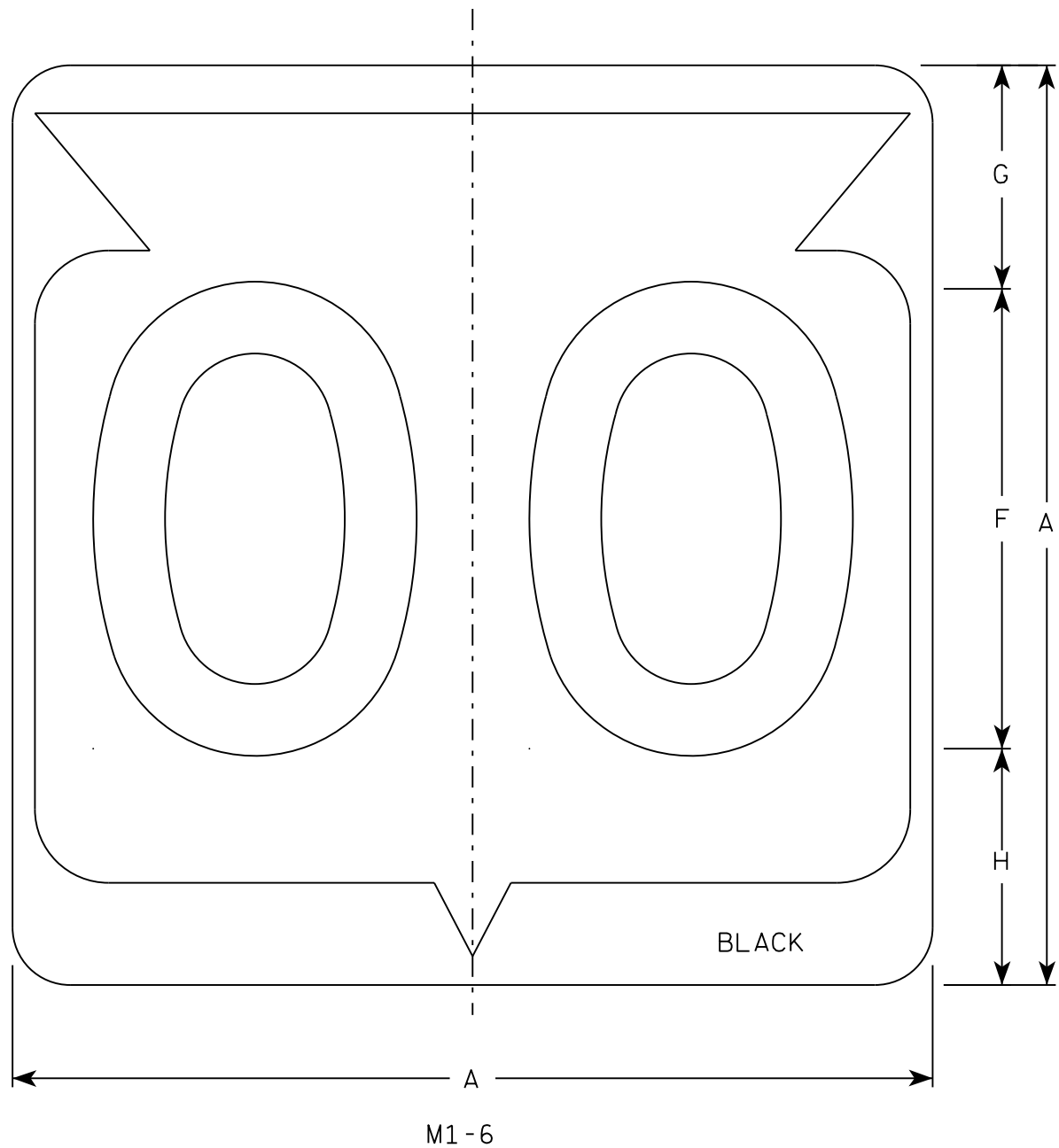
DATE 7/12/07 PLATE NO. A5-10.1

PROJECT NO:

SHEET NO:

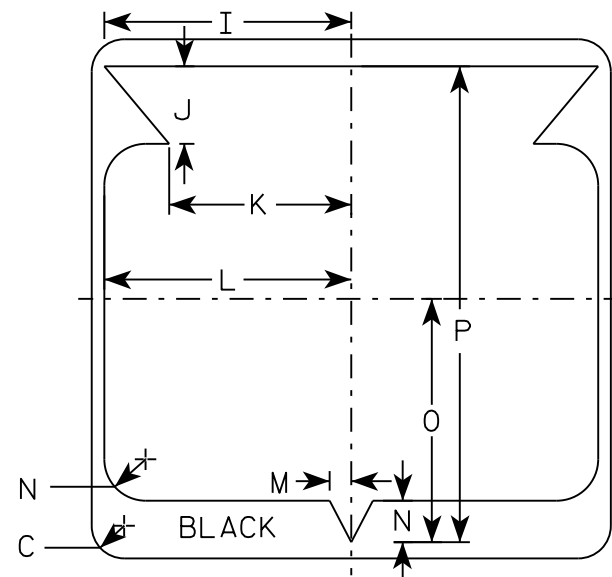
E

7



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|---|---|----|-------|-------|--------|-------|--------|--------|-------|-------|--------|--------|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | | 1 1/2 | | | 12 | 5 1/2 | 6 1/2 | 10 1/4 | 2 1/2 | 8 7/8 | 11 1/2 | 1 | 1 7/8 | 11 1/4 | 21 7/8 | | | | | | | | | | | 4.0 |
| 3 | 36 | | 2 1/4 | | | 18 | 8 3/4 | 9 1/4 | 15 3/8 | 5 3/8 | 12 5/8 | 17 1/8 | 1 1/2 | 2 7/8 | 16 7/8 | 33 | | | | | | | | | | | 9.0 |
| 4 | 36 | | 2 1/4 | | | 18 | 8 3/4 | 9 1/4 | 15 3/8 | 5 3/8 | 12 5/8 | 17 1/8 | 1 1/2 | 2 7/8 | 16 7/8 | 33 | | | | | | | | | | | 9.0 |
| 5 | 36 | | 2 1/4 | | | 18 | 8 3/4 | 9 1/4 | 15 3/8 | 5 3/8 | 12 5/8 | 17 1/8 | 1 1/2 | 2 7/8 | 16 7/8 | 33 | | | | | | | | | | | 9.0 |

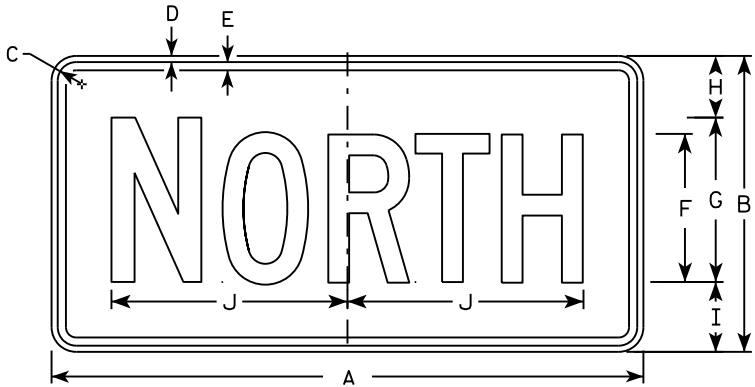
| | | | | |
|-------------|------|---------|-----------|---|
| PROJECT NO: | HWY: | COUNTY: | SHEET NO: | E |
|-------------|------|---------|-----------|---|

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

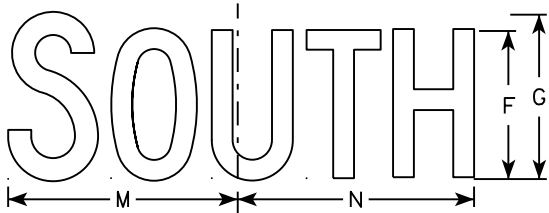
DATE 3/16/18 PLATE NO. M1-6.10



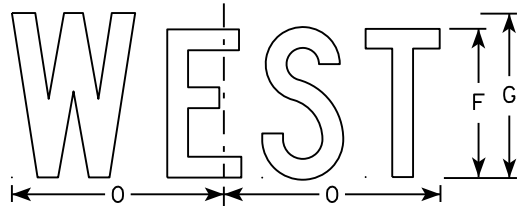
M3-1
MM3-1
MP3-1



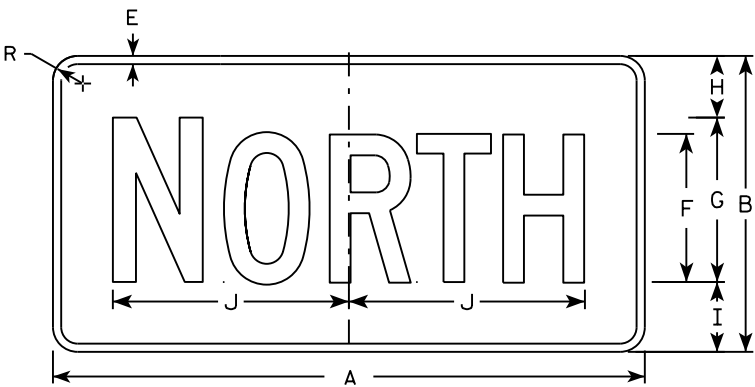
M3-2
MM3-2
MP3-2



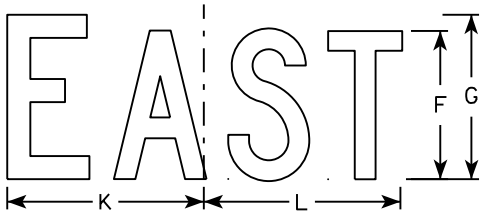
M3-3
MM3-3
MP3-3



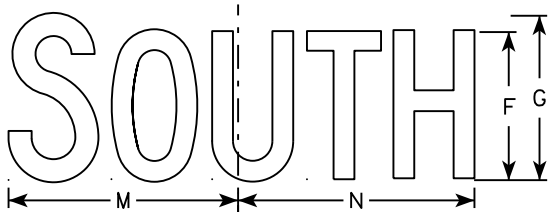
M3-4
MM3-4
MP3-4



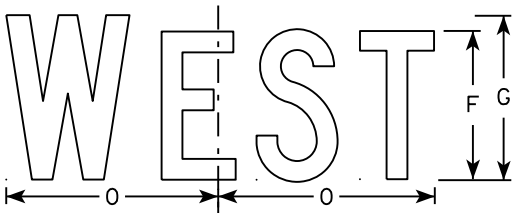
MB3-1
MK3-1
MN3-1



MB3-2
MK3-2
MN3-2



MB3-3
MK3-3
MN3-3



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|----|-------|-------|--------|-------|--------|--------|--------|-------|---|---|-------|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | 12 | 1 1/8 | 3/8 | 3/8 | 6 | 7 | 2 1/4 | 2 3/4 | 10 1/4 | 7 7/8 | 8 3/8 | 10 1/4 | 9 3/4 | 8 3/4 | | | 1 1/2 | | | | | | | | | 2.00 |
| 3 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 10 | 3 3/4 | 4 1/4 | 14 3/8 | 12 | 12 1/8 | 14 | 14 1/8 | 13 | | | 1 1/2 | | | | | | | | | 4.5 |
| 4 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 10 | 3 3/4 | 4 1/4 | 14 3/8 | 12 | 12 1/8 | 14 | 14 1/8 | 13 | | | 1 1/2 | | | | | | | | | 4.5 |
| 5 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 10 | 3 3/4 | 4 1/4 | 14 3/8 | 12 | 12 1/8 | 14 | 14 1/8 | 13 | | | 1 1/2 | | | | | | | | | 4.5 |

STANDARD SIGNS
M3-1 thru M3-4
SERIES

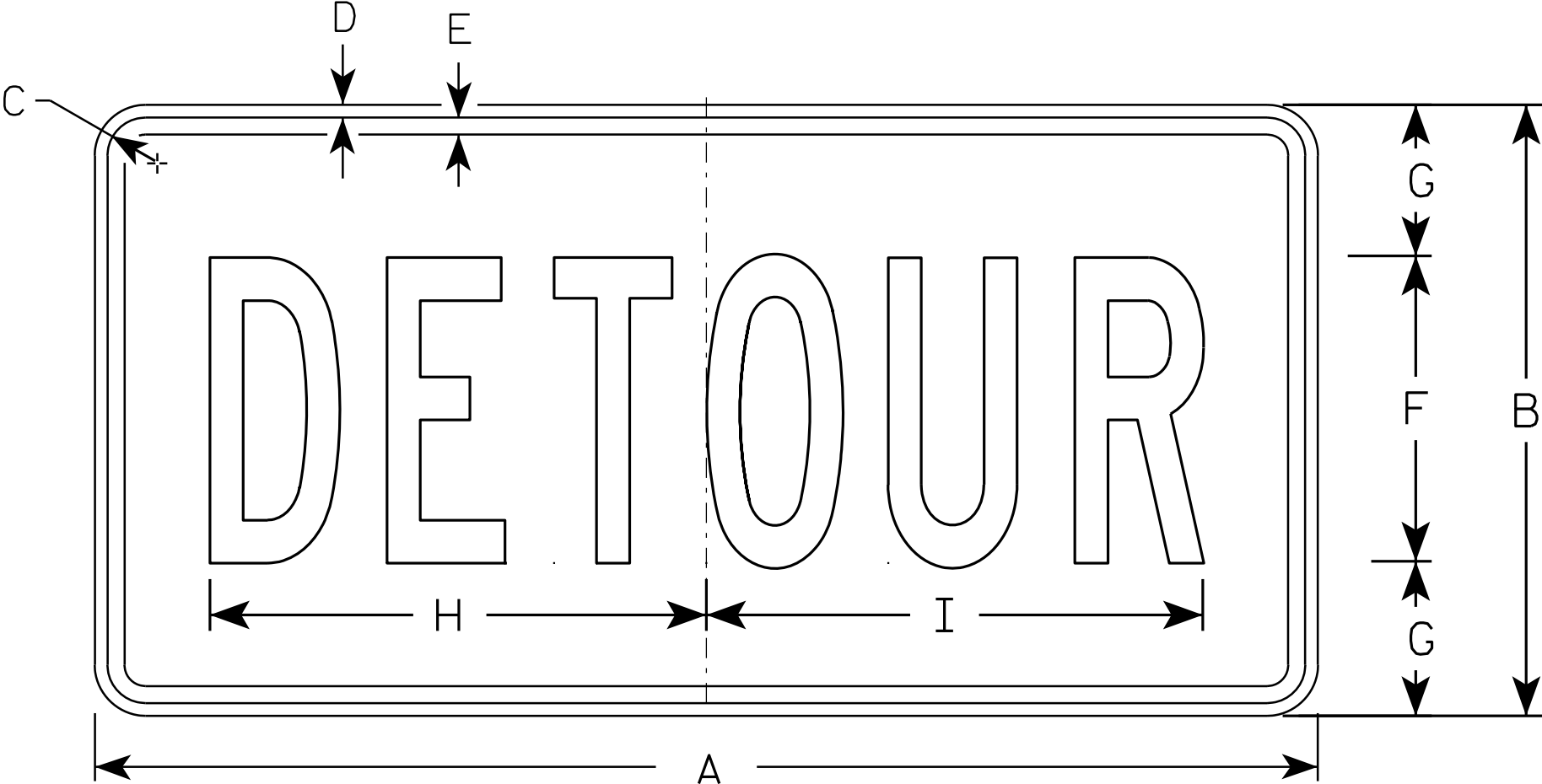
WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
 - Background - Orange
 - Message - Black
- 3. Message Series - B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4 - 8

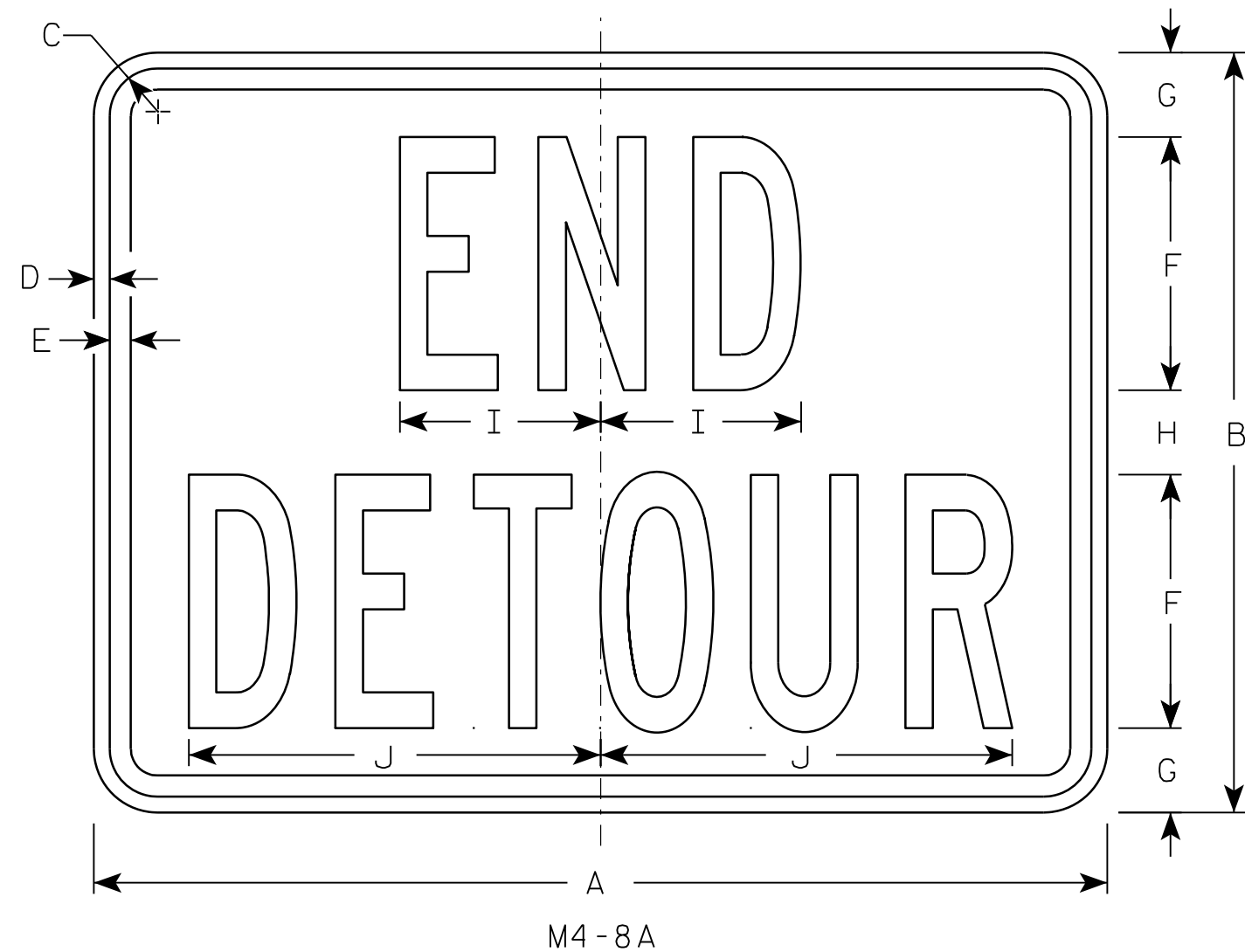
| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|--------|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | 12 | 1 1/8 | 3/8 | 3/8 | 6 | 3 | 10 | 10 1/4 | | | | | | | | | | | | | | | | | | 2.0 |
| 3 | 36 | 18 | 1 1/8 | 3/8 | 1/2 | 9 | 4 1/2 | 14 5/8 | 14 1/2 | | | | | | | | | | | | | | | | | | 4.5 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
M4 - 8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|---|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 24 | 18 | 1 1/8 | 3/8 | 1/2 | 6 | 2 | 2 | 4 3/4 | 9 3/4 | | | | | | | | | | | | | | | | | 3.0 |
| 3 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 8 | 2 1/2 | 3 | 6 3/4 | 13 | | | | | | | | | | | | | | | | | 5.0 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

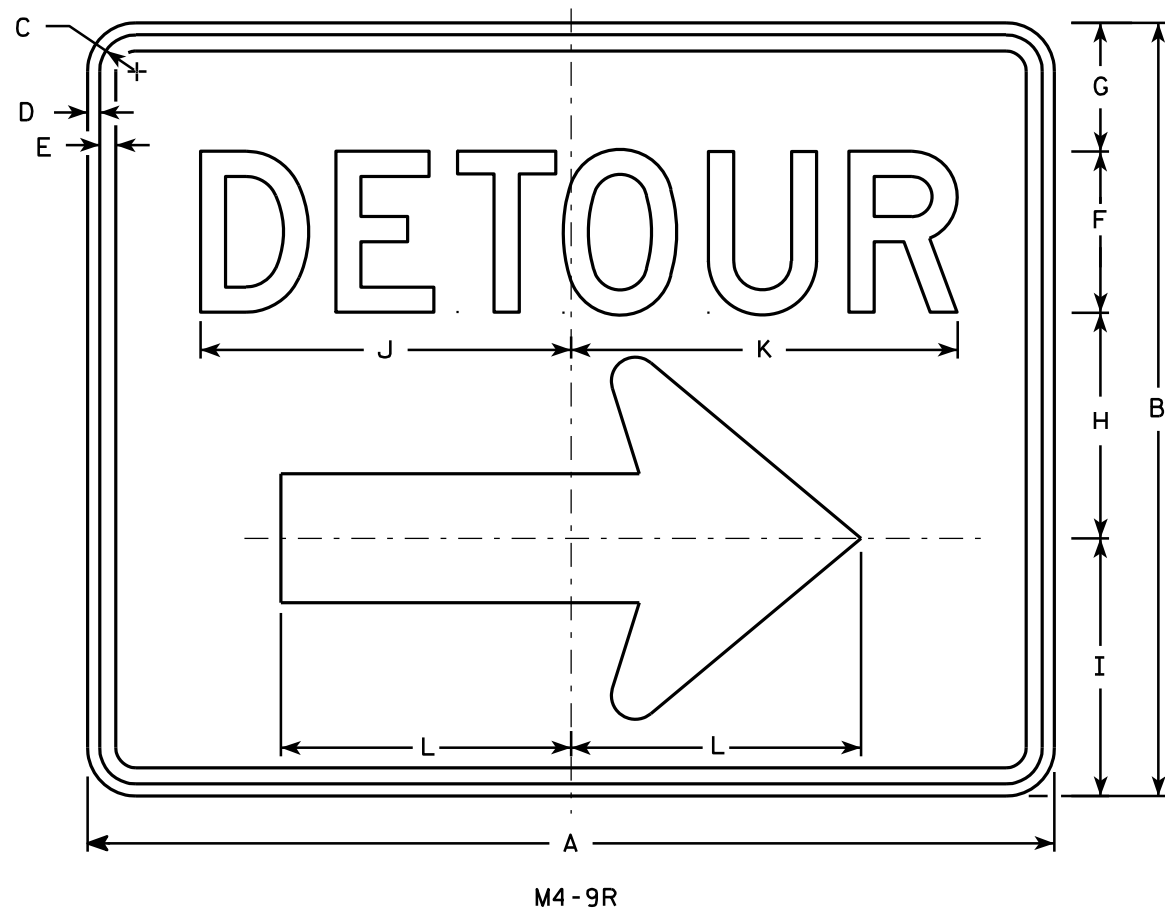
| | | | | |
|-------------|------|---------|-----------|---|
| PROJECT NO: | HWY: | COUNTY: | SHEET NO: | E |
|-------------|------|---------|-----------|---|

STANDARD SIGN
M4-8A

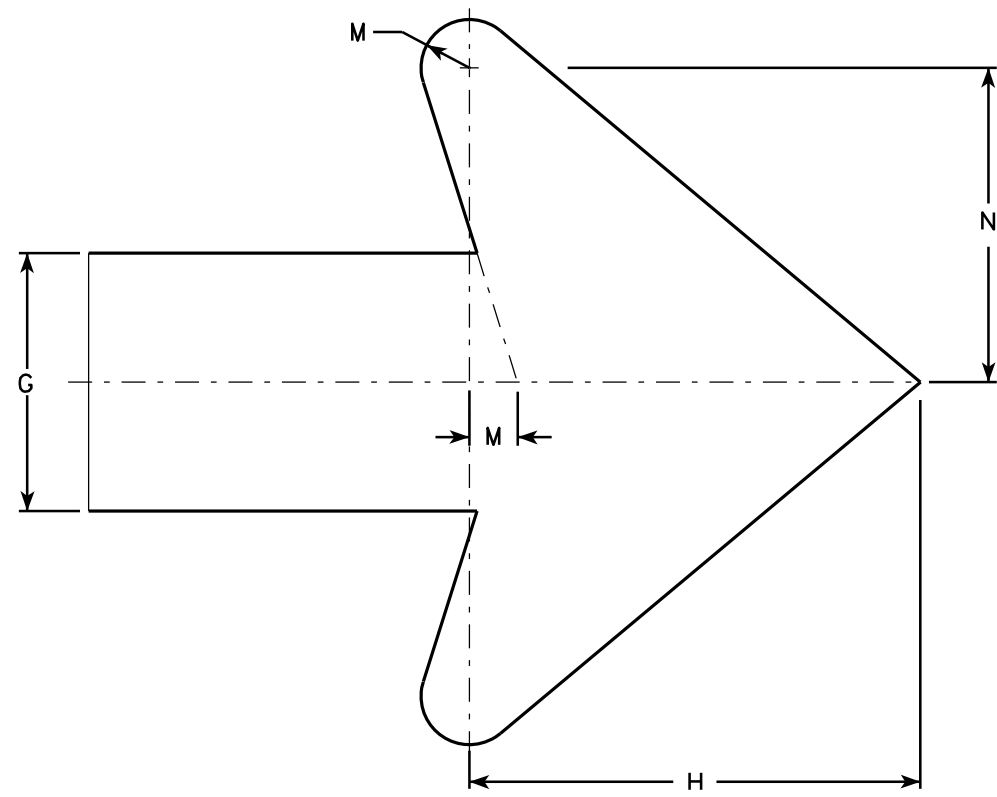
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2



- NOTES**
1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
 2. Color:
Background - Orange
Message - Black
 3. Message Series - D
 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
 5. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|--------|--------|--------|--------|--------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 4 | 7 | 8 | 11 1/2 | 12 | 9 | 3/4 | 4 7/8 | | | | | | | | | | | | | 5.00 |
| 3 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 4 | 7 | 8 | 11 1/2 | 12 | 9 | 3/4 | 4 7/8 | | | | | | | | | | | | | 5.00 |
| 4 | 48 | 36 | 1 3/8 | 1/2 | 5/8 | 8 | 6 | 10 1/2 | 11 5/8 | 20 5/8 | 20 1/2 | 13 1/4 | 1 1/8 | 6 7/8 | | | | | | | | | | | | | 12.0 |
| 5 | 48 | 36 | 1 3/8 | 1/2 | 5/8 | 8 | 6 | 10 1/2 | 11 5/8 | 20 5/8 | 20 1/2 | 13 1/4 | 1 1/8 | 6 7/8 | | | | | | | | | | | | | 12.0 |

STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

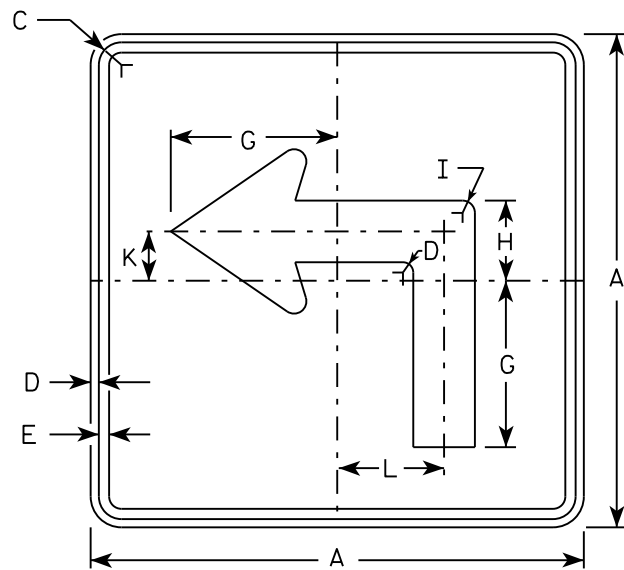
PROJECT NO:

HWY:

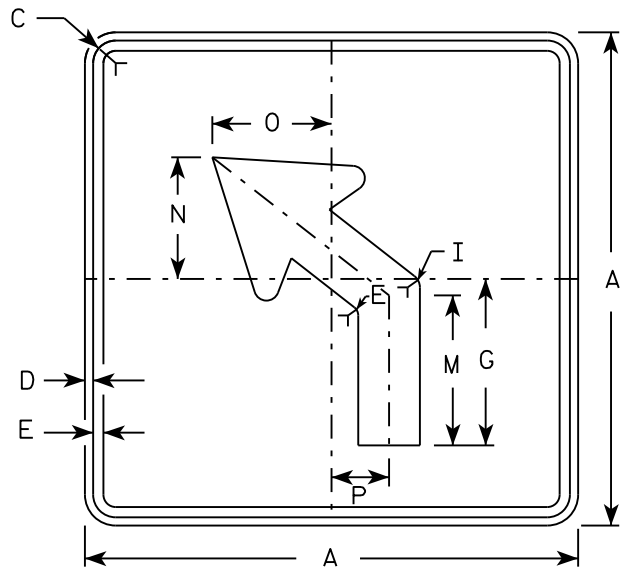
COUNTY:

SHEET NO:

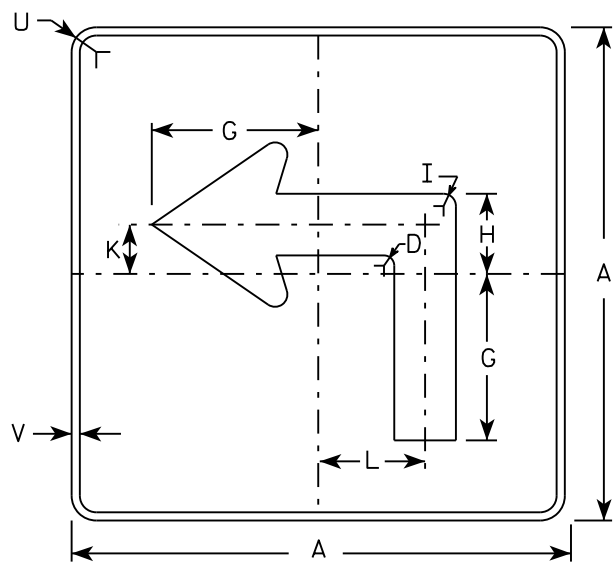
E



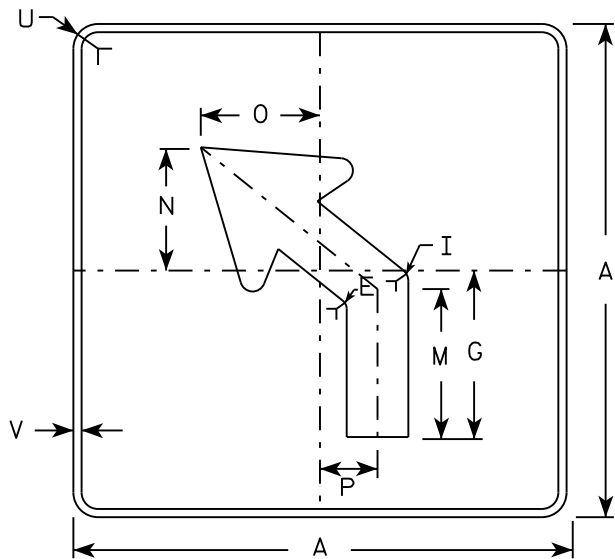
M5-1L
MM5-1L
M05-1L
MP5-1L



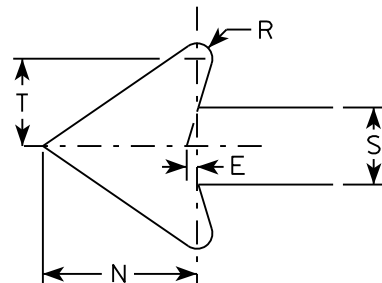
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|-----------------------------------------|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White - Type H Reflective |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

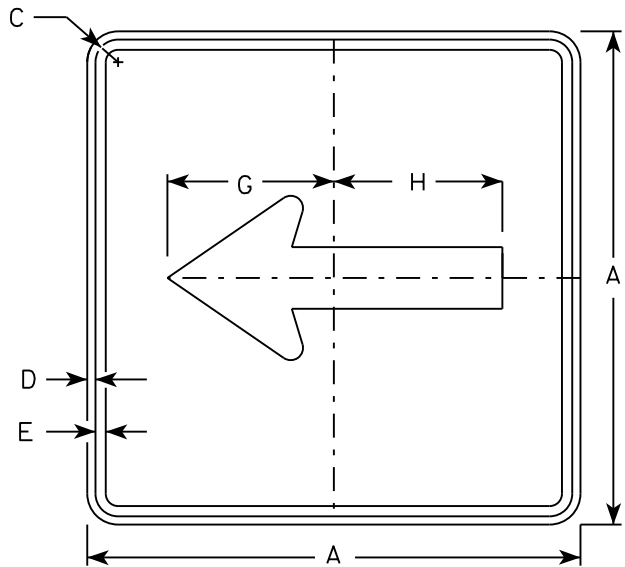
| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|--------|-------|-----|---|-------|-------|-------|-------|-------|-------|---|-----|-------|-------|-------|-----|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 21 | | 1 1/8 | 3/8 | 3/8 | | 7 | 3 3/8 | 5/8 | | 2 1/8 | 4 1/2 | 6 3/8 | 5 1/4 | 5 | 2 1/2 | | 1/2 | 2 5/8 | 3 | 1 1/2 | 1/2 | | | | | 3.06 |
| 3 | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 1/8 | 4 7/8 | 7/8 | | 3 | 6 1/2 | 9 1/8 | 7 1/2 | 7 1/4 | 3 1/2 | | 3/4 | 3 3/4 | 4 1/4 | 1 7/8 | 1/2 | | | | | 6.25 |
| 4 | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 1/8 | 4 7/8 | 7/8 | | 3 | 6 1/2 | 9 1/8 | 7 1/2 | 7 1/4 | 3 1/2 | | 3/4 | 3 3/4 | 4 1/4 | 1 7/8 | 1/2 | | | | | 6.25 |
| 5 | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 1/8 | 4 7/8 | 7/8 | | 3 | 6 1/2 | 9 1/8 | 7 1/2 | 7 1/4 | 3 1/2 | | 3/4 | 3 3/4 | 4 1/4 | 1 7/8 | 1/2 | | | | | 6.25 |

STANDARD SIGN
M5-1 & M5-2

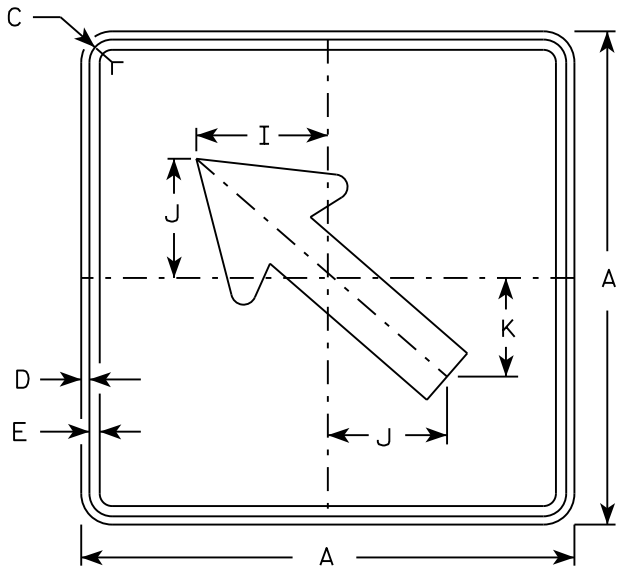
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

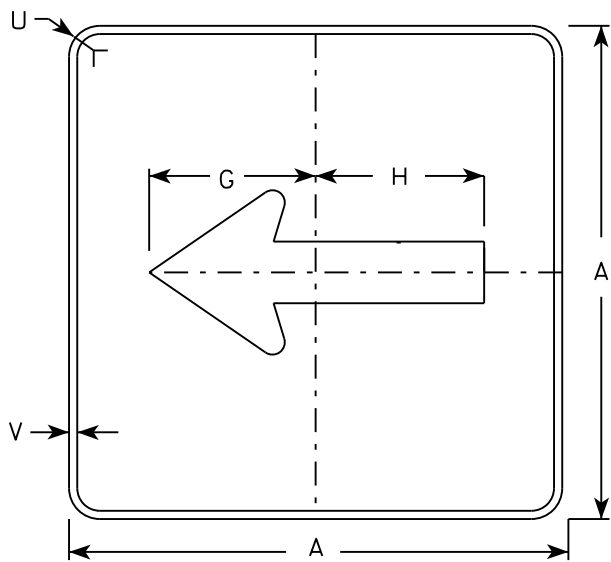
DATE 10/15/15 PLATE NO. M5-1.13



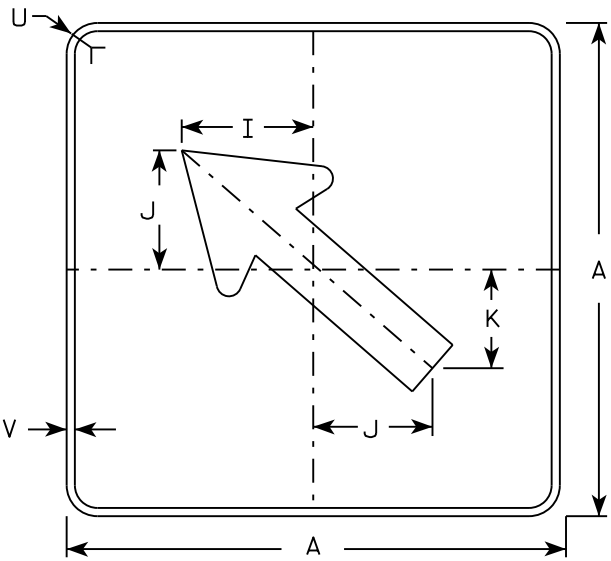
M6 - 1
MM6 - 1
M06 - 1
MP6 - 1



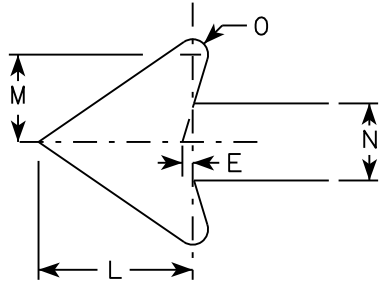
M6 - 2
MM6 - 2
M06 - 2
MP6 - 2



MB6 - 1
MK6 - 1
MN6 - 1
MR6 - 1



MB6 - 2
MK6 - 2
MN6 - 2
MR6 - 2



NOTES

- 1. Signs are Type II - Type H except as Shown
- 2. Color:
Background - See note 4
Message - See note 4
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|--------|--------|-------|-------|-------|-------|-------|-------|-----|---|---|---|---|---|-------|-----|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 21 | | 1 1/8 | 3/8 | 3/8 | | 7 1/2 | 7 1/8 | 5 5/8 | 5 | 4 1/4 | 5 1/4 | 3 | 2 5/8 | 1/2 | | | | | | 1 1/2 | 1/2 | | | | | 3.06 |
| 3 | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 3/4 | 10 1/4 | 8 | 7 1/4 | 6 | 7 1/2 | 4 1/4 | 3 3/4 | 3/4 | | | | | | 1 7/8 | 1/2 | | | | | 6.25 |
| 4 | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 3/4 | 10 1/4 | 8 | 7 1/4 | 6 | 7 1/2 | 4 1/4 | 3 3/4 | 3/4 | | | | | | 1 7/8 | 1/2 | | | | | 6.25 |
| 5 | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 3/4 | 10 1/4 | 8 | 7 1/4 | 6 | 7 1/2 | 4 1/4 | 3 3/4 | 3/4 | | | | | | 1 7/8 | 1/2 | | | | | 6.25 |

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

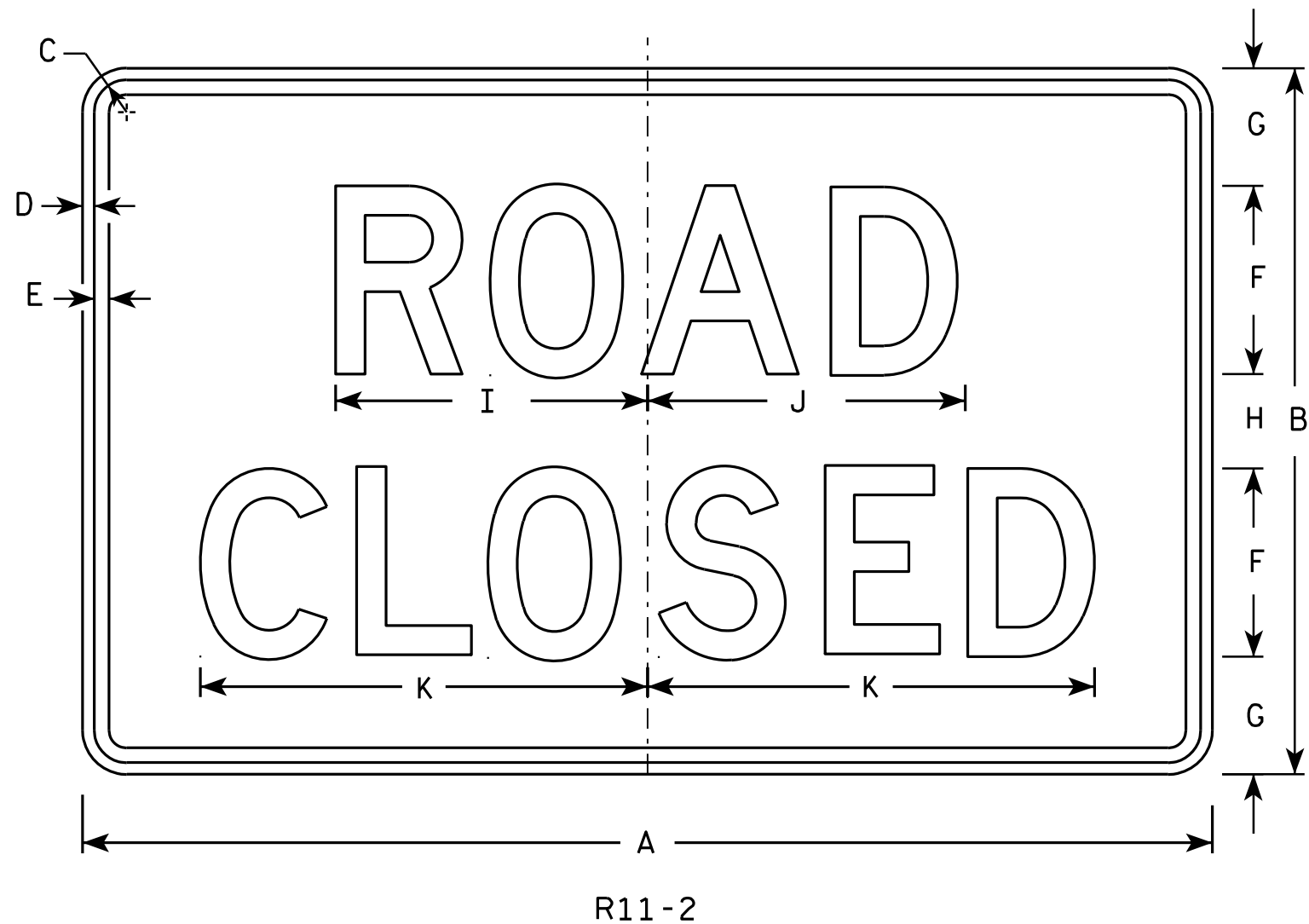
E

STANDARD SIGN
M6 - 1 & M6 - 2
SERIES

WISCONSIN DEPT OF TRANSPORTATION

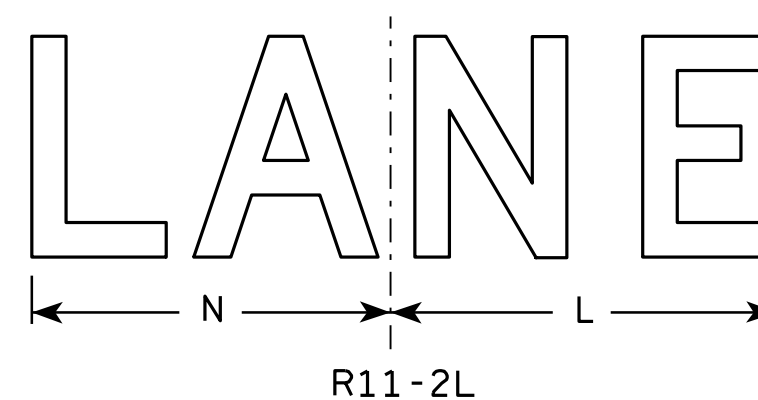
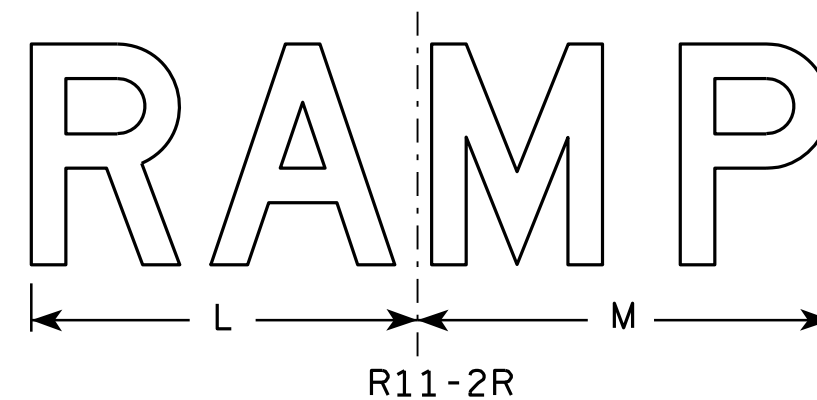
APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15



NOTES

- Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - White
Message - Black
- Message Series - D
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- Modify the message as required.



| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|---|--------|--------|----|----|----|----|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 48 | 30 | 1 3⁄8 | 1⁄2 | 5⁄8 | 8 | 5 | 4 | 13 1⁄4 | 13 1⁄2 | 19 | 14 | 15 | 13 | | | | | | | | | | | | | 10.0 |
| 2M | 48 | 30 | 1 3⁄8 | 1⁄2 | 5⁄8 | 8 | 5 | 4 | 13 1⁄4 | 13 1⁄2 | 19 | 14 | 15 | 13 | | | | | | | | | | | | | 10.0 |
| 3 | 48 | 30 | 1 3⁄8 | 1⁄2 | 5⁄8 | 8 | 5 | 4 | 13 1⁄4 | 13 1⁄2 | 19 | 14 | 15 | 13 | | | | | | | | | | | | | 10.0 |
| 4 | 48 | 30 | 1 3⁄8 | 1⁄2 | 5⁄8 | 8 | 5 | 4 | 13 1⁄4 | 13 1⁄2 | 19 | 14 | 15 | 13 | | | | | | | | | | | | | 10.0 |
| 5 | 48 | 30 | 1 3⁄8 | 1⁄2 | 5⁄8 | 8 | 5 | 4 | 13 1⁄4 | 13 1⁄2 | 19 | 14 | 15 | 13 | | | | | | | | | | | | | 10.0 |

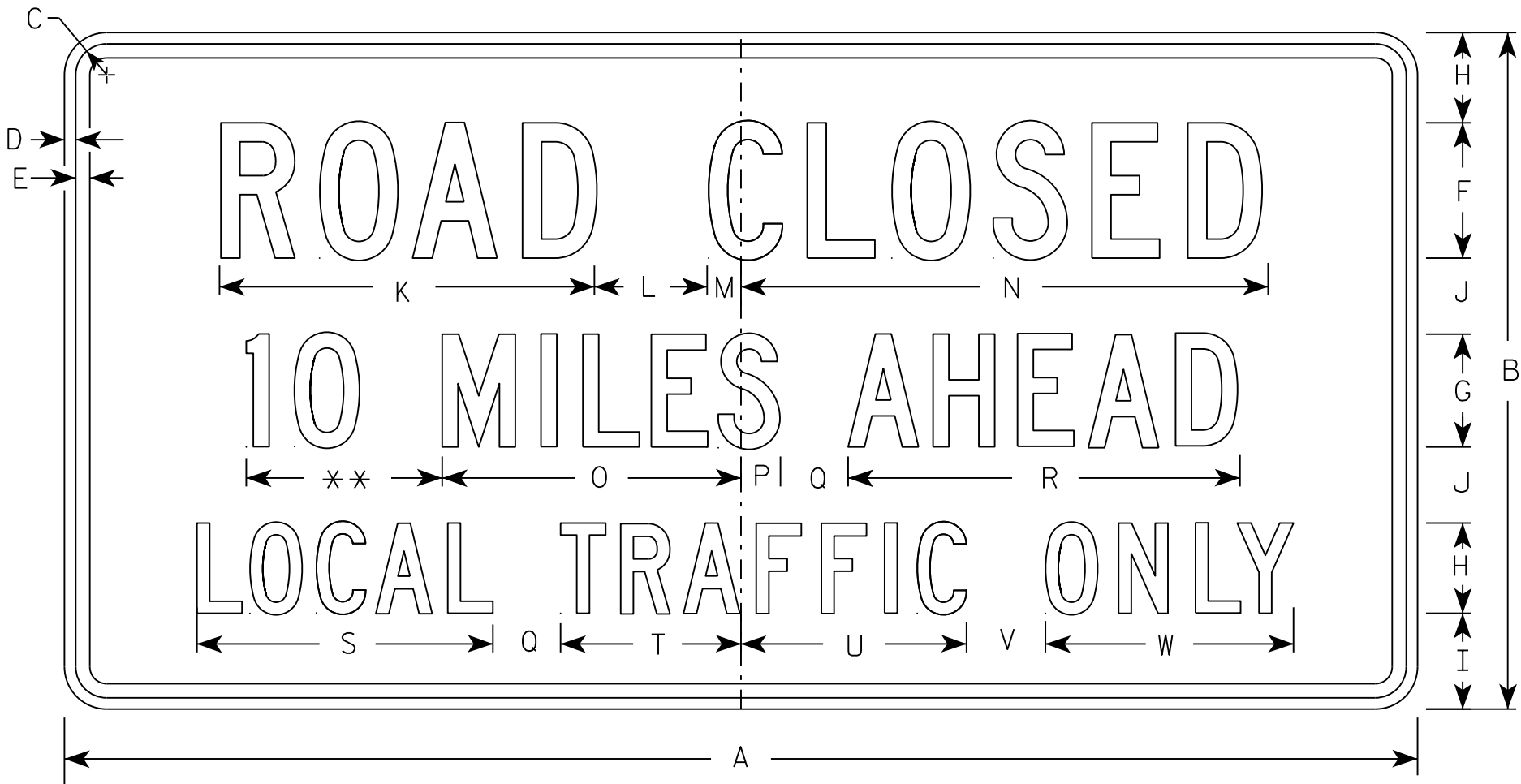
STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 4/1/11 PLATE NO. R11-2.10

PROJECT NO: HWY: COUNTY: SHEET NO: E

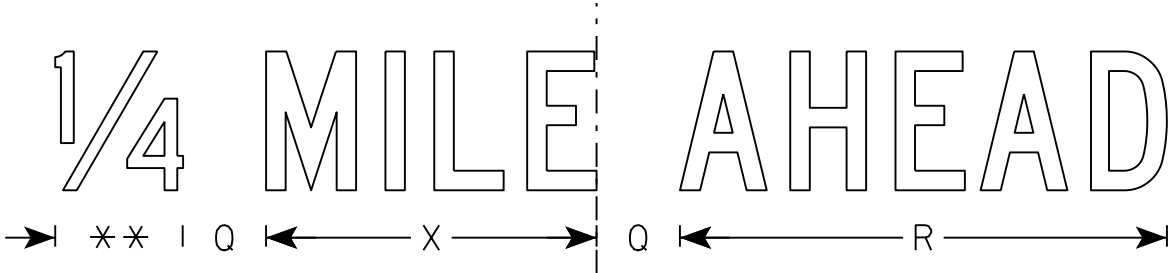
NOTES

- 1. Sign is Type II - Type H Reflective
- 2. Color:
 - Background - White
 - Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5

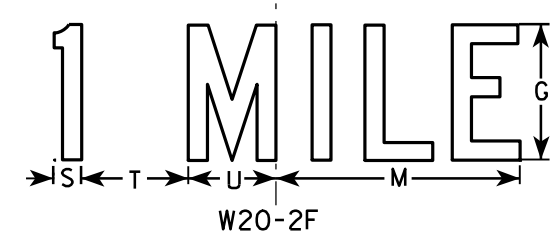
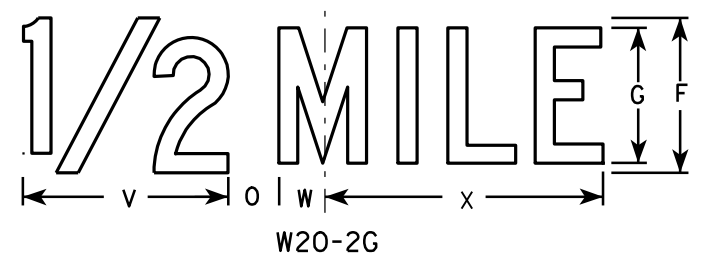
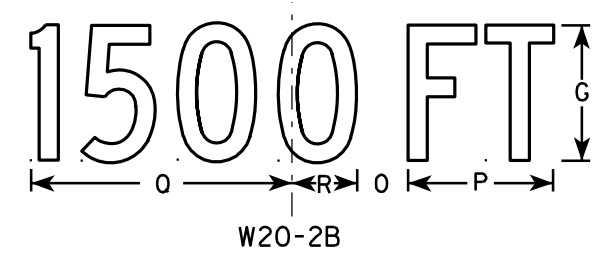
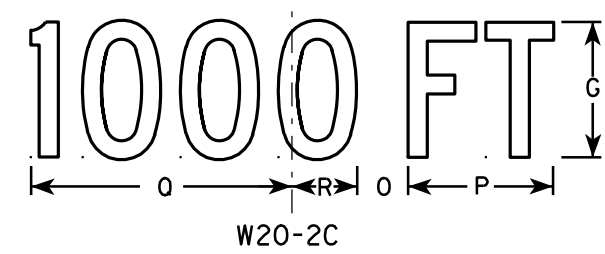
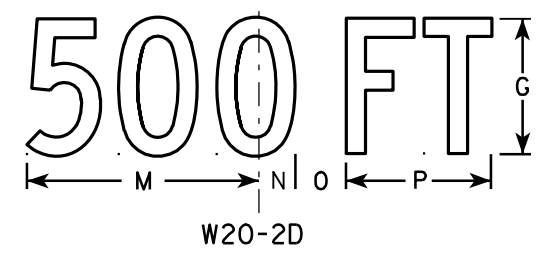
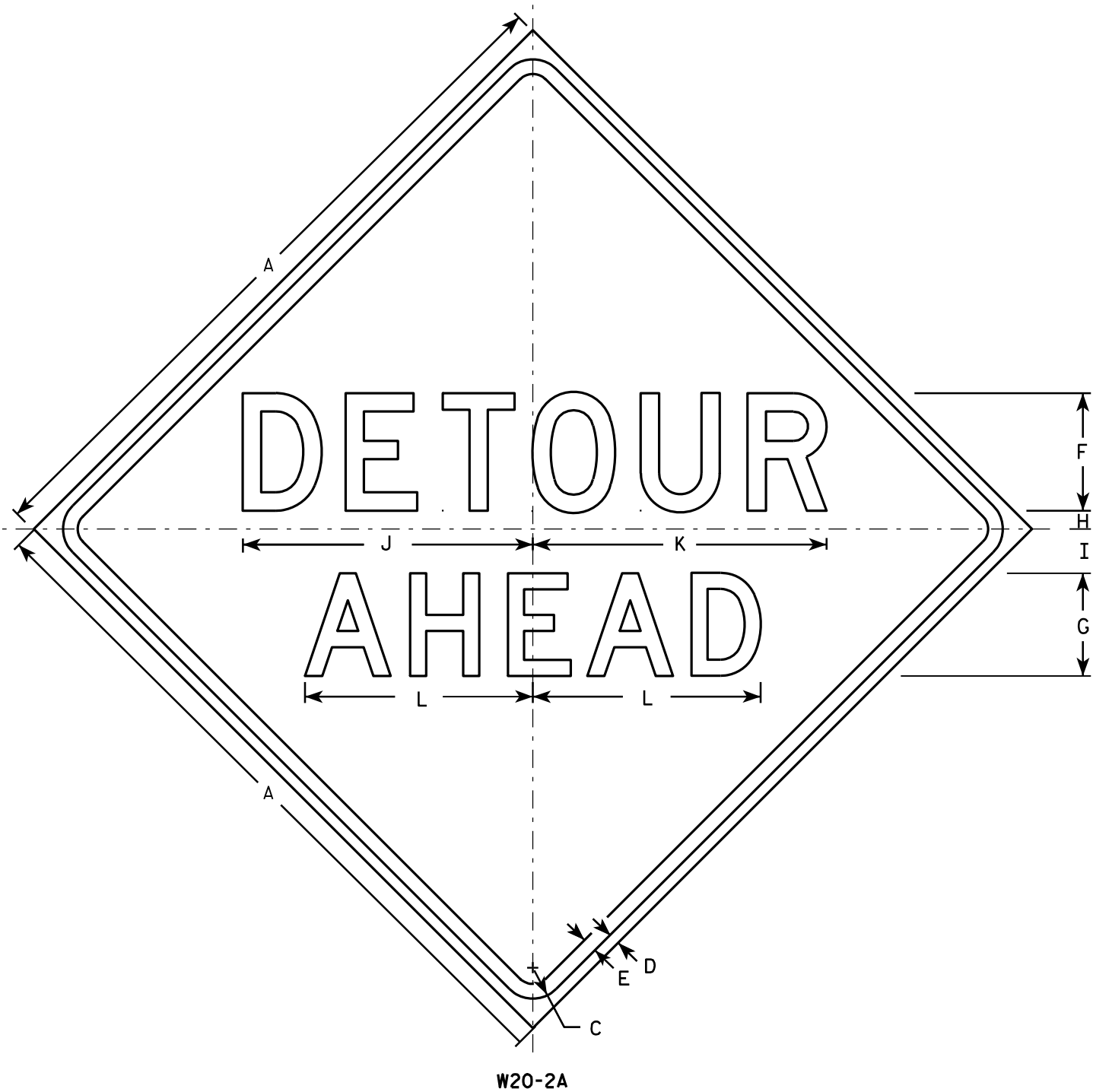


| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|-------|-------|-------|--------|---|-------|--------|--------|-------|---|--------|--------|-------|-------|-------|-------|--------|---|---|-----------------|
| 1 | 36 | 18 | 1 3⁄8 | 1⁄2 | 5⁄8 | 4 | 3 | 2 1⁄2 | 2 | 2 | 11 1⁄8 | 3 | 1 1⁄8 | 15 1⁄4 | 8 | 1 1⁄2 | 2 | 10 3⁄4 | 8 3⁄8 | 4 3⁄4 | 6 1⁄2 | 2 | 6 3⁄4 | 7 1⁄8 | | | 4.5 |
| 2S | 60 | 30 | 1 3⁄8 | 1⁄2 | 5⁄8 | 6 | 5 | 4 | 4 1⁄4 | 3 3⁄8 | 16 5⁄8 | 5 | 1 1⁄2 | 23 | 13 1⁄4 | 1 3⁄4 | 3 | 17 3⁄8 | 13 1⁄8 | 8 | 10 | 3 1⁄2 | 11 | 11 7⁄8 | | | 12.5 |
| 2M | 60 | 30 | 1 3⁄8 | 1⁄2 | 5⁄8 | 6 | 5 | 4 | 4 1⁄4 | 3 3⁄8 | 16 5⁄8 | 5 | 1 1⁄2 | 23 | 13 1⁄4 | 1 3⁄4 | 3 | 17 3⁄8 | 13 1⁄8 | 8 | 10 | 3 1⁄2 | 11 | 11 7⁄8 | | | 12.5 |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 3/15/17 PLATE NO. R11-3.8



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|---|-------|-------|--------|----|--------|----|-------|-------|-------|--------|-------|-------|-------|-------|--------|-------|--------|---|---|-----------------|
| 1 | 36 | | 1 5/8 | 5/8 | 3/4 | 6 | 5 | 1 | 2 1/4 | 14 3/4 | 15 | 11 5/8 | 9 | 1 3/8 | 1 7/8 | 5 5/8 | 10 1/8 | 2 1/2 | 1 1/8 | 4 1/2 | 3 1/2 | 8 | 1 3/4 | 10 3/4 | | | 9.0 |
| 2S | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 2M | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 3 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 4 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 5 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |

STANDARD SIGN
W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

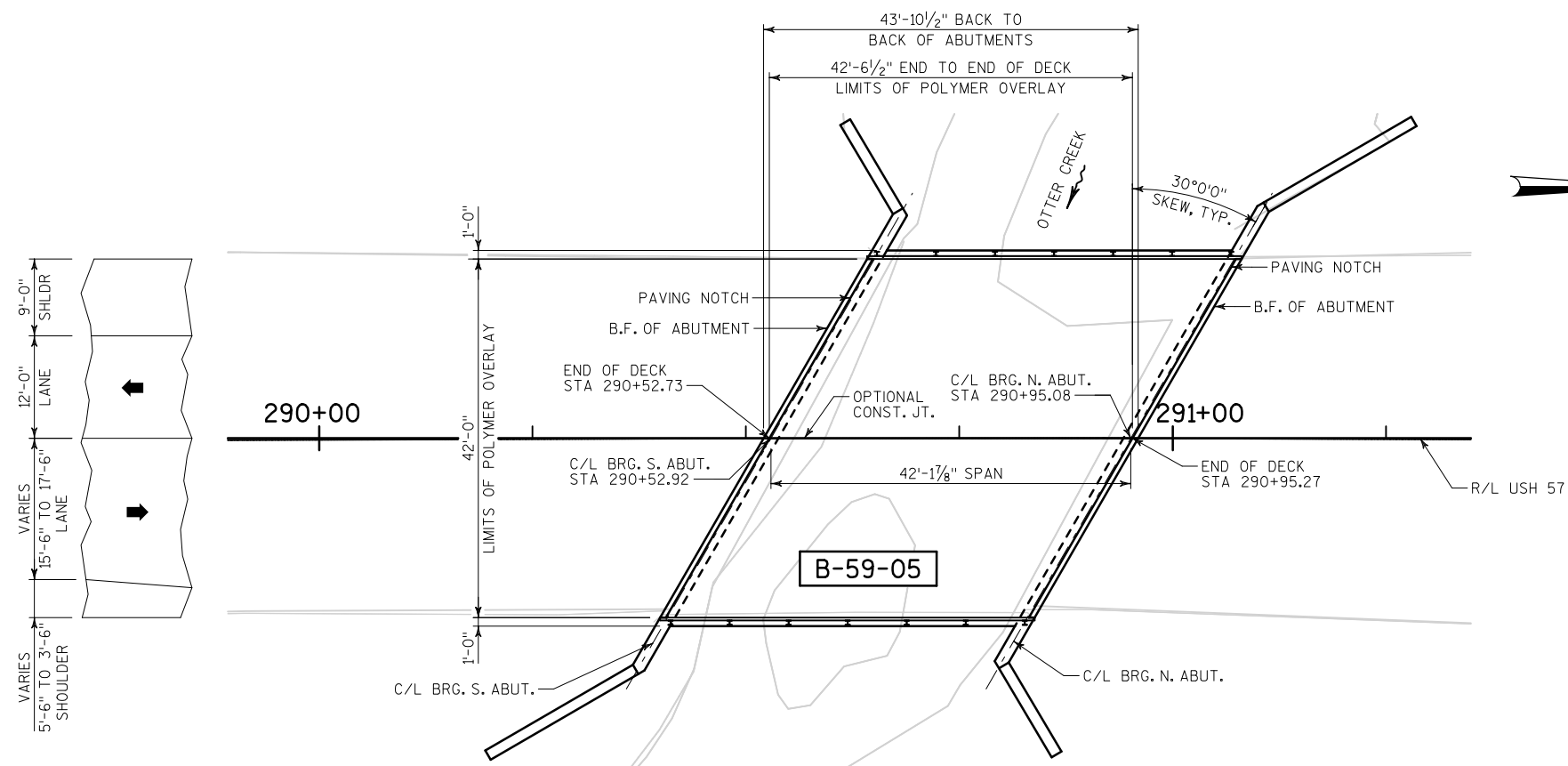
LIVE LOAD:

| | |
|---------------------------------------------|----------|
| DESIGN LOADING: HS-20 | |
| INVENTORY RATING: | HS-30 |
| OPERATING RATING: | HS-50 |
| WISCONSIN STANDARD PERMIT VEHICLE (WISSPV): | 250 KIPS |

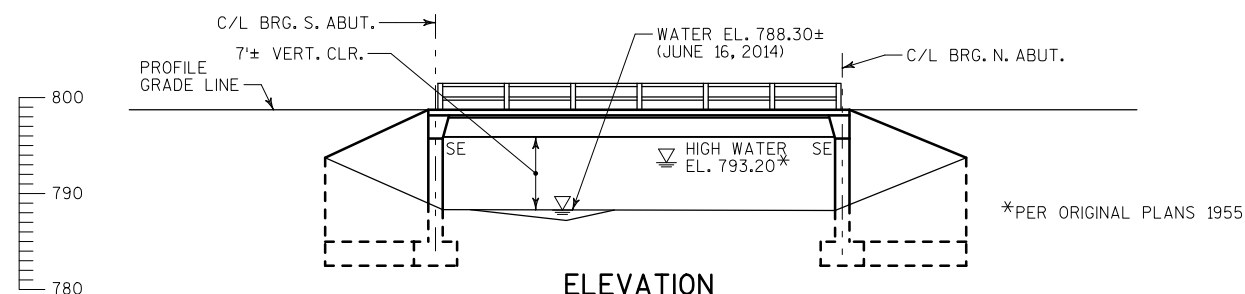
STH 57

AADT = 7,200 (2014)
AADT = 8,800 (2030)
RDS = 55 MPH

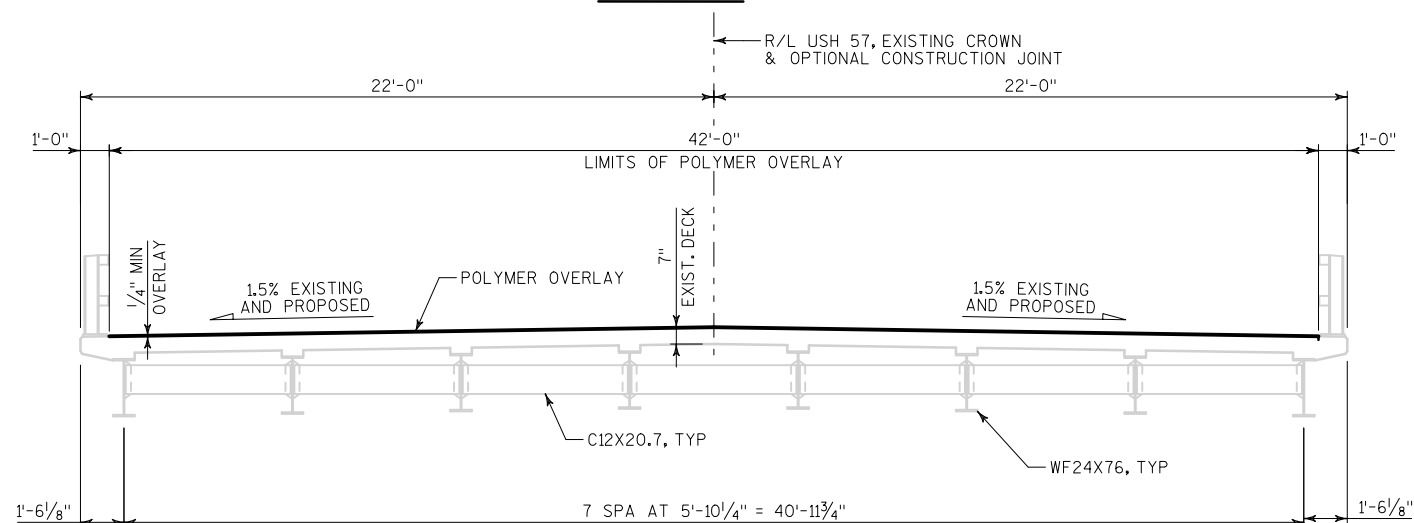
1. GENERAL PLAN, ELEVATION & CROSS-SECTION
2. GENERAL NOTES & QUANTITIES



(POLYMER OVERLAY AND PAINTING OF 1-SPAN STEEL ROLLED BEAM GIRDER)



ELEVATION



(LOOKING EAST)



STRUCTURES DESIGN CONTACTS

BRIDGE OFFICE:
WILLIAM DREHER (608) 266-8489
CONSULTANT:
RANDY THOMAS (414) 847-0447

| | | | |
|-----|------|----------|----|
| | | | |
| NO. | DATE | REVISION | BY |

ch2m. hill

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED William C. Dreher SDR 02/20/18
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-59-05

STATE HIGHWAY 57 OVER OTTER CREEK

| | | | |
|--------|-----------|-------------------|----------|
| COUNTY | SHEBOYGAN | TOWN/CITY/VILLAGE | PLYMOUTH |
|--------|-----------|-------------------|----------|

| | | | |
|--------------|-----|--------------------|-----|
| DESIGN SPEC. | | REHABILITATION N/A | |
| DESIGNED BY | ASW | DESIGN CK'D. | WRT |
| DRAWN BY | TEK | PLANS CK'D. | ASW |

GENERAL PLAN,
ELEVATION &
CROSS-SECTION

SHEET 1 OF 2

TOTAL ESTIMATED QUANTITIES

| CATEGORY NO. | ITEM NO. | BID ITEMS | UNIT | TOTAL |
|--------------|------------|-------------------------------------------------------------------------|------|-------|
| 0010 | 509.5100.S | POLYMER OVERLAY | SY | 199 |
| 0020 | 517.1800.S | STRUCTURE REPAINTING RECYCLED ABRASIVE B-59-05 | LS | 1 |
| 0020 | 517.4500.S | NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-59-05 | LS | 1 |
| 0020 | 517.6001.S | PORTABLE DECONTAMINATION FACILITY | EACH | 1 |
| | | | | |

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.

ALL DIMENSIONS ARE IN FEET AND INCHES UNLESS OTHERWISE NOTED.

ALL STATIONS AND ELEVATIONS ARE IN FEET. ELEVATIONS REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD).

THE CONTRACTOR IS RESPONSIBLE FOR MAKING THEIR OWN DETERMINATION AS TO TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE. UTILITIES LABELED AS PROPOSED MAY BE INSTALLED BY OTHERS PRIOR TO THIS CONTRACT.

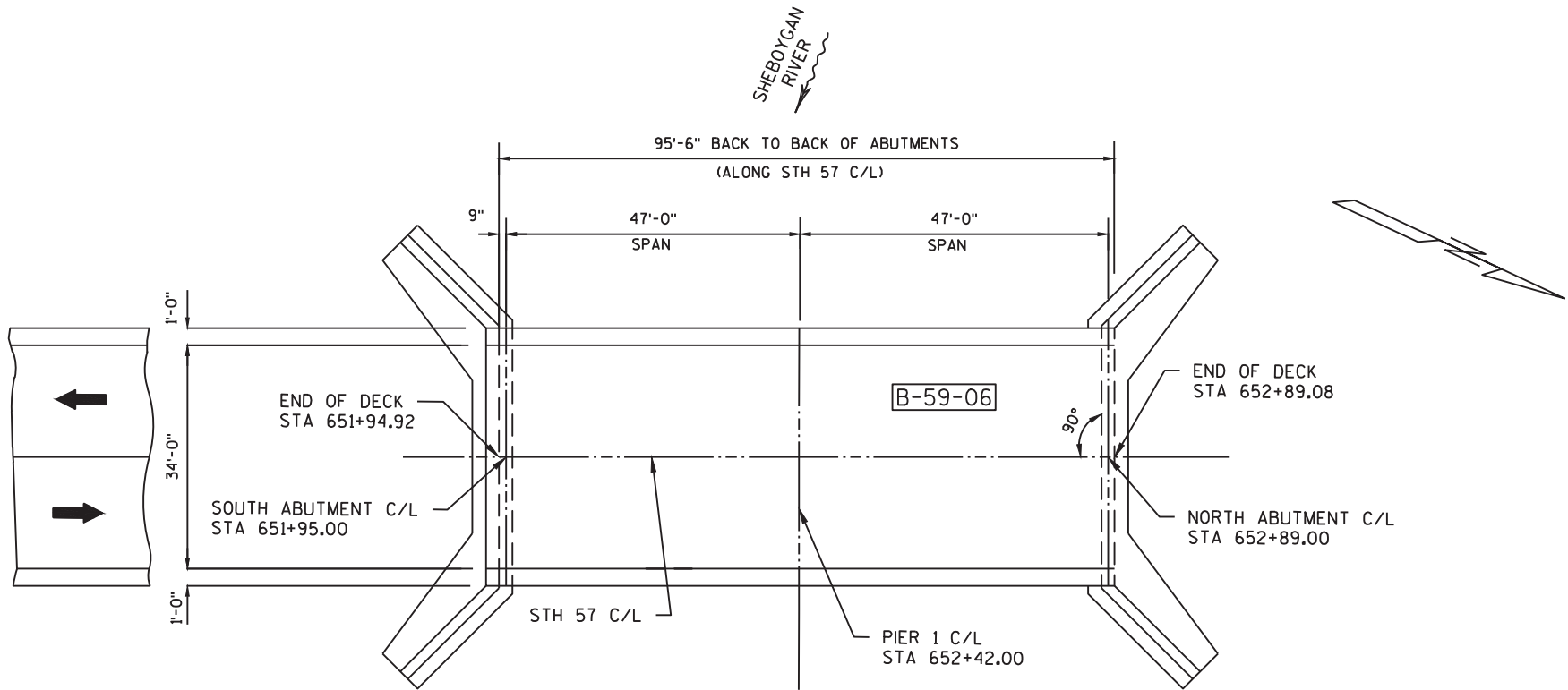
DECK SURFACE PREPARATION, INCLUDING THE REMOVAL OF THE EXISTING EPOXY OVERLAY, IS INCLUDED UNDER THE BID ITEM "POLYMER OVERLAY".

CLEAN & PAINT ALL EXPOSED STEEL SURFACES UNDERNEATH THE BRIDGE DECK. THE SURFACES INCLUDE GIRDERS, DIAPHRAGMS, WEB STIFFENERS AND SPLICE PLATES.

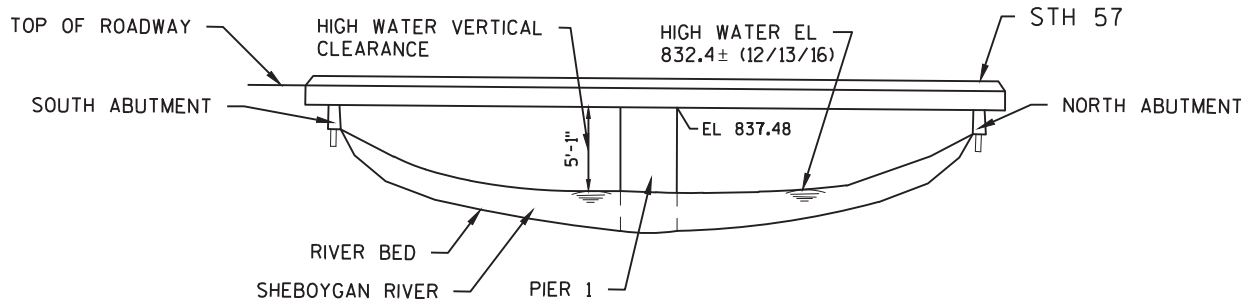
BRIDGE B-59-05 WILL BE CLOSED TO LIVE TRAFFIC DURING CONSTRUCTION. NO STAGING IS REQUIRED.

PAINT SHALL BE FEDERAL STANDARD COLOR NO. 25240 BLUE.

| | | | |
|----------------------------------------------------|------|----------|-----------------|
| | | | |
| NO. | DATE | REVISION | BY |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-59-05 | | | |
| DRAWN BY | | ASW | PLANS CK'D. WRT |
| GENERAL NOTES & QUANTITIES | | | SHEET 2 OF 2 |
| | | | |



PLAN B-59-6



ELEVATION B-59-6

TRAFFIC DATA

AADT = 3,400 (2016)
AADT = 6,720 (2023)
RDS = 55 MPH

DESIGN DATA


LIVE LOAD (TAKEN FROM HSI - 10/24/2017)
DESIGN LOADING: HS=20
INVENTORY RATING: HS=20
OPERATING RATING: HS=33
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV)=250 (KIPS)

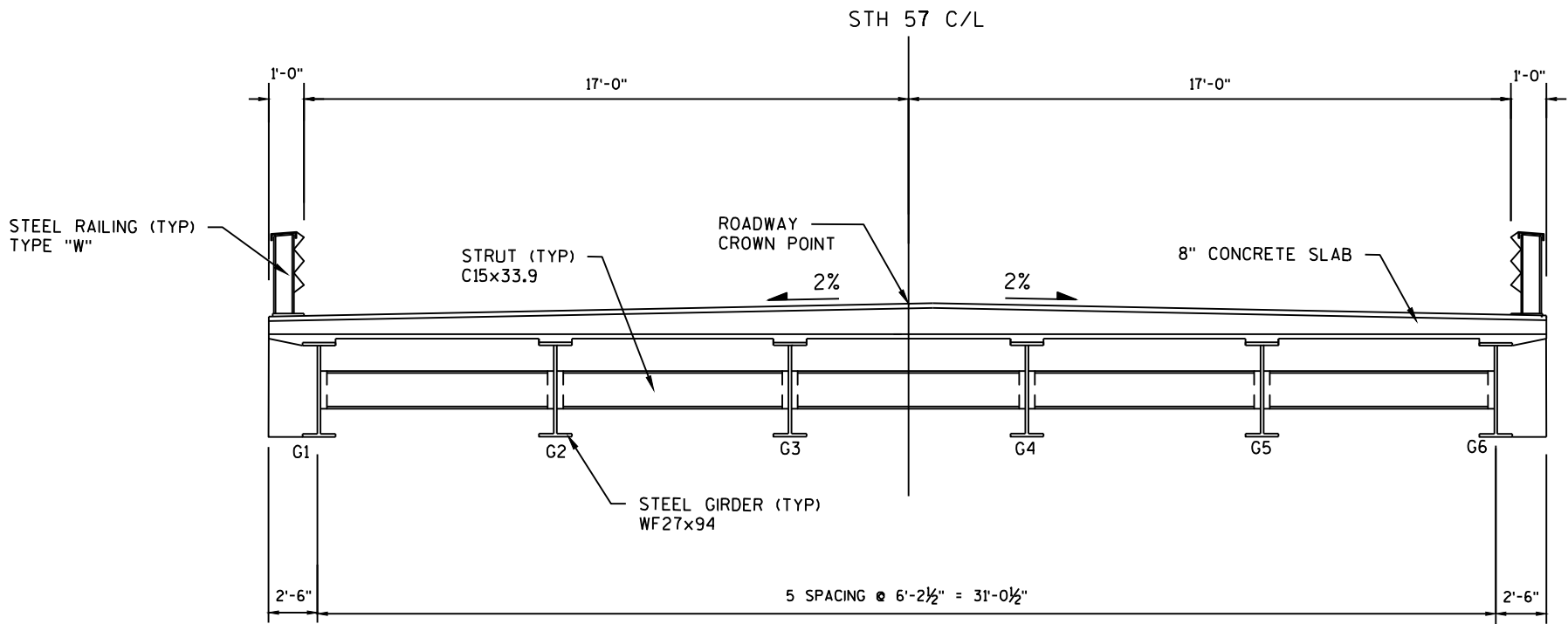
CONTACTS

NE REGION PROJECT MANAGER
ERIC GWIDT (920) 366-8896
BUREAU OF STRUCTURES
WILLIAM DREHER (608) 266-8489

LIST OF DRAWINGS

- GENERAL PLAN & ELEVATION
- CROSS SECTION, GENERAL NOTES & QUANTITIES

| | | | |
|---------------------------------------------------------------------------------------|-----------|---------------------------------|--------------|
|  | | Plans Prepared By WISDOT | |
| ACCEPTED <i>William C. Dreher</i> SDR | | 02/08/18 | |
| CHIEF STRUCTURES DESIGN ENGINEER | | DATE | |
| STRUCTURE B-59-06 | | | |
| STH 57 OVER SHEBOYGAN RIVER | | | |
| COUNTY | SHEBOYGAN | TOWN | RHINE |
| DESIGN SPEC. REHABILITATION N/A | | | |
| DESIGNED BY | JAK | DESIGN CK'D. | ESG |
| DRAWN BY | JBS | PLANS CK'D. | JAK |
| GENERAL PLAN, ELEVATION & CROSS-SECTION | | | SHEET 1 OF 2 |



CROSS SECTION THRU ROADWAY
(LOOKING NORTH)

NOTES

DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE EXISTING ORIGINAL STRUCTURE PLANS.
SEE HIGHWAY STRUCTURE INFORMATION SYSTEM FOR ADDITIONAL DETAIL.
PAINT COLOR SHALL BE FEDERAL STANDARD COLOR NO. 20122 BROWN.
ALL EXPOSED STEEL SURFACES UNDERNEATH THE BRIDGE ARE TO BE
CLEANED AND PAINTED. THE SURFACES INCLUDE GIRDERS, CROSS FRAMING,
DIAPHRAGMS, STIFFENERS, BEARINGS AND CONNECTIONS.

TOTAL ESTIMATED QUANTITIES

| CATEGORY NO. | ITEM NO. | BID ITEMS | UNIT | TOTAL |
|--------------|------------|-------------------------------------------------------------------------|------|-------|
| 0030 | 517.1800.S | STRUCTURE REPAINTING RECYCLED ABRASIVE B-59-06 | LS | 1 |
| 0030 | 517.4500.S | NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-59-06 | LS | 1 |
| 0030 | 517.6001.S | PORTABLE DECONTAMINATION FACILITY | EACH | 1 |

FOR INFORMATION ONLY: APPROXIMATELY 4,850 SF OF STRUCTURAL STEEL

| | | | |
|----------------------------------------------------|------|-------------|--------------|
| NO. | DATE | REVISION | BY |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-59-06 | | | |
| DRAWN BY | JBS | PLANS CK'D. | JAK |
| GENERAL NOTES & QUANTITIES | | | SHEET 2 OF 2 |



Wisconsin Department of Transportation

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