LAX

DECEMBER 2018

PROJECT ID: 5539-00-70

WITH: N/A

COUNTY: VERNON

ORDER OF SHEETS

TOTAL SHEETS = 34

N

DESIGN DESIGNATION 5539-00-70

A.A.D.T.	2019	= 200
A.A.D.T.	2039	= 230
D.H.V.		= 34
D.D.		= 60/40
T.		= 5.0%
DESIGN SPEED		= 40 M.P.H.
ESALS		= 22,000

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- HIGH VOLTAGE (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- OVERHEAD UTILITY (OH)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

CTH WW - CTH W

(SEYMOUR CREEK BRIDGE B-62-0257)

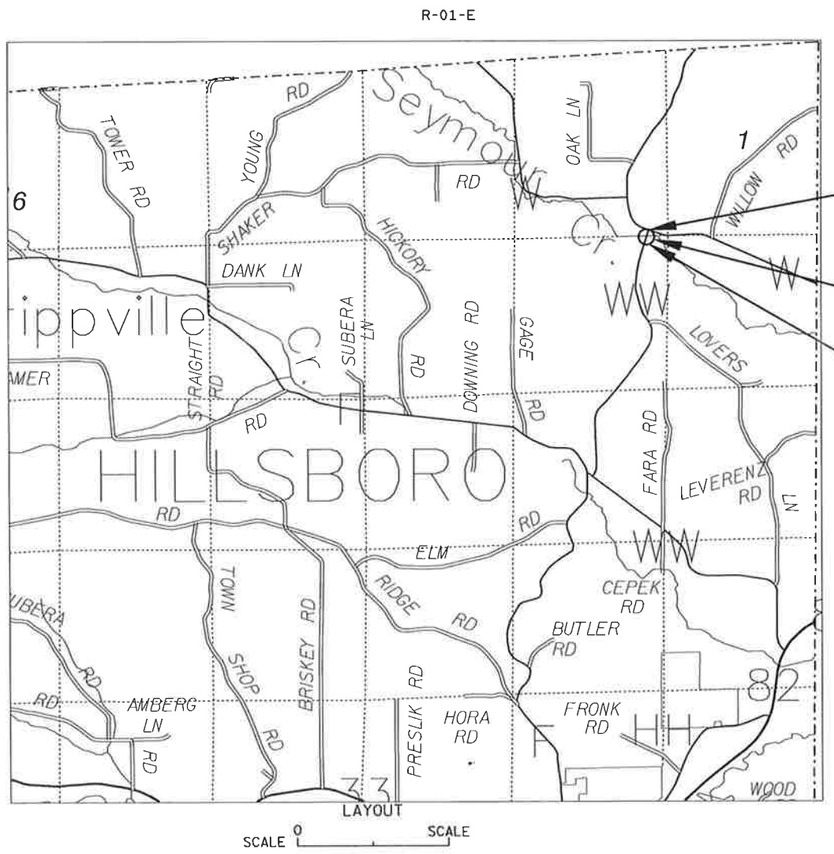
CTH WW

VERNON COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5539-00-70	———	—

STATE PROJECT NUMBER

5539-00-70



END PROJECT
STA. 14+00
Y = 206 648.512
X = 848 676.466

PROJECT LOCATION
B-62-0257
T-14-N

BEGIN PROJECT
STA. 12+00
Y = 206 456.345
X = 848 621.044

LAYOUT SCALE 0 SCALE

TOTAL NET LENGTH OF CENTERLINE = 0.0038 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, VERNON COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ACCEPTED FOR

COUNTY of VERNON

Hwy. Comm.

7-5-18 (Date) (Signature & Title of Official)

ORIGINAL PLANS PREPARED BY

WESTBROOK
Associated Engineers, Inc.
619 EAST HOXIE STREET
P.O. BOX 429
SPRING GREEN, WISCONSIN 53588
PHONE (608) 588-7866
FAX (608) 588-7954

WISCONSIN PROFESSIONAL ENGINEER
AARON B. PALMER
E-35695
RICHLAND CENTER, WI

7-7-18 (Date) (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WESTBROOK
Designer	WESTBROOK
Management Consultant	KL ENGINEERING, INC.

APPROVED FOR THE DEPARTMENT

DATE: 7/25/18 Management Consultant Signature

E

GENERAL NOTES

EROSION CONTROL ITEMS TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER. SILT FENCE SHALL BE IN PLACE PRIOR TO CONSTRUCTION. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NEEDED.

EROSION CONTROL FEATURES AS SHOWN ON THE PLANS ARE SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

DISTURBED AREAS WITHIN THE RIGHT–OF–WAY, EXCEPT THE DRIVING LANES AND THE SHOULDERS ARE TO BE FERTILIZED, SEEDED, TEMPORARY SEEDED, AND MULCHED AS DIRECTED BY THE ENGINEER.

RESTORATION OF EXPOSED SLOPES AND DITCHES SHALL TAKE PLACE WITHIN 7 CALENDAR DAYS AFTER FINISHED GRADING IS COMPLETE.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

WETLANDS ARE PRESENT AT THE LOCATIONS SHOWN IN THE PLANS. DO NOT OPERATE MACHINERY OUTSIDE OF THE SLOPE INTERCEPTS IN THESE LOCATIONS.

REMOVAL OF ASPHALTIC SURFACES WHERE AN ABUTTING ASPHALTIC SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A SAWCUT MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

EXACT LOCATION AND WIDTH OF ALL DRIVEWAY ENTRANCES TO BE DETERMINED BY THE ENGINEER. DRIVEWAYS SHALL BE REPLACED IN KIND.

NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY UTILITY WHICH IS NOT A MEMBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR IS RESPONSIBLE FOR FIELD LOCATING ALL UTILITIES.

ASPHALTIC SURFACE LAYERS:
- UPPER: 1¾"
- LOWER: 2¼"

RUNOFF COEFFICIENT TABLE

HYDROLOGIC SOIL GROUP												
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE	0–2	2–6	6 & OVER	0–2	2–6	6 & OVER	0–2	2–6	6 & OVER	0–2	2–6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP–TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE–TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 – .95											
CONCRETE	.80 – .95											
BRICK	.70 – .80											
DRIVES,WALKS	.75 – .85											
ROOFS	.75 – .95											
GRAVEL ROADS, SHOULDERS	.40 – .60											

TOTAL PROJECT AREA = 0.30 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.24 ACRES

CONTACTS

CONSULTANT LIAISON
WESTBROOK ASSOCIATED ENGINEERS, INC.
619 EAST HOXIE STREET
SPRING GREEN, WI 53588

ATTN: AARON PALMER, P.E.
PH: (608) 588–7866
FAX: (608) 588–7954
apalmer@westbrookeng.com

COUNTY LIAISON
VERNON COUNTY HIGHWAY DEPARTMENT
602 NORTH MAIN STREET
P.O. BOX 232
VIROQUA, WI 54665

ATTN: PHIL HEWITT
PH: (608) 637–5452
phil.hewitt@vernoncounty.org

WisDNR LIAISON
DNR SERVICE CENTER
3550 MORMON COULEE ROAD
LA CROSSE, WI 54601

ATTN: KAREN KALVELAGE
PH: (608) 785–9115
karen.kalvelage@wisconsin.gov

UTILITIES

ELECTRIC
VERNON ELECTRIC COOP
ATTN: COLE CARY
110 SAUGSTAD RD.
WESTBY, WI 54667
(608)–634–3121
ccary@vernonelectric.com

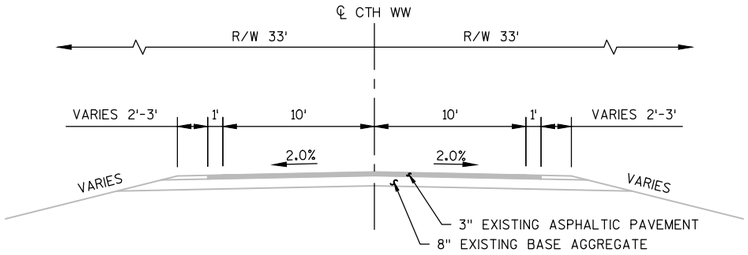
COMMUNICATIONS
CENTURYLINK
ATTN: BRET CLARK
311 SOUTH COURT STREET
SPARTA, WI 54656
(608)–269–0819
bret.clark@centurylink.com

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

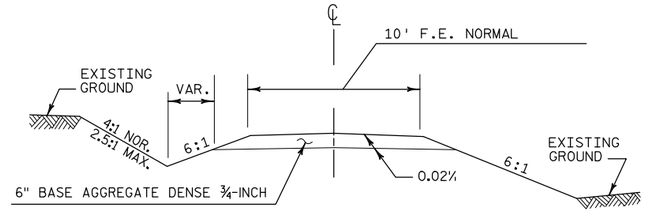
* DENOTES UTILITY IS NOT A MEMBER OF DIGGERS HOTLINE

STANDARD ABBREVIATIONS

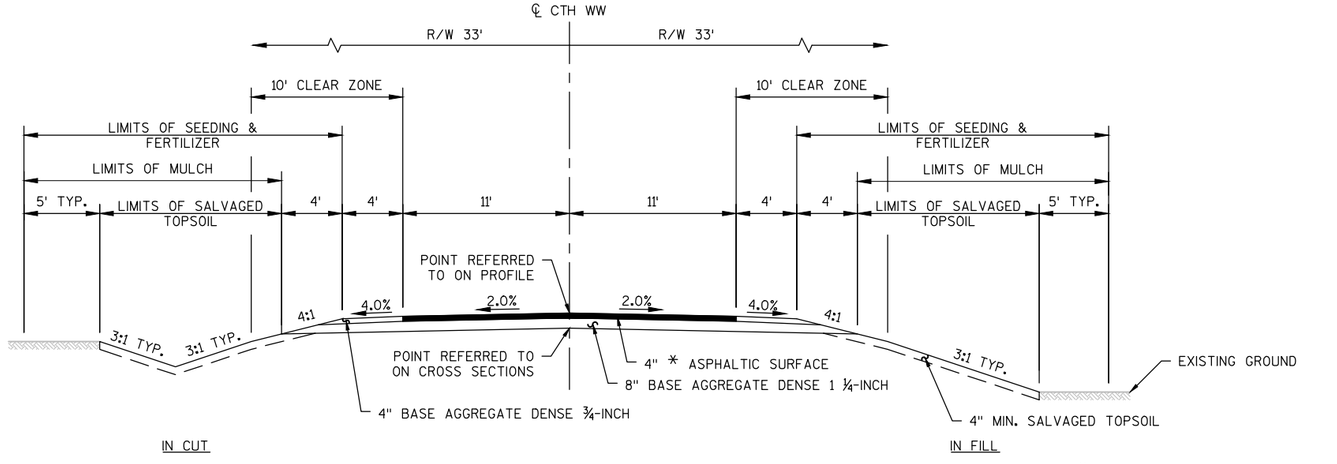
ABUT.	Abutment	JT	Joint	SEC	Section
AC	Acre	JCT	Junction	SHLDR	Shoulder
AGG.	Aggregate	LHF	Left–Hand Forward	SHR	SHRINKAGE
AH	Ahead	L	Length of Curve	SW	Sidewalk
<	Angle	LIN FT OR LF	Linear Foot	S	South
ASPH	Asphaltic	LC	Long Chord of Curve	SQ	Square
AVG.	Average	MH	Manhole	SF OR SQ FT	Square Feet
A.D.T	Average Daily Traffic	MB	Mailbox	SY or SQ YD	Square Yard
BAD	Base Aggregate Dense	ML OR M/L	Match Line	STD	Standard
BK	Back	N	North	SDD	Standard Detail Drawings
BF	Back Face	Y	North Grid Coordinate	STH	State Trunk Highway
B.M.	Bench Mark	OD	Outside Diameter	STA	Station
BR.	Bridge	PLE	Permanent Limited Easement	SS	Storm Sewer
C/L	Center Line	PT	Point	SG	Subgrade
CC	Center to Center	PC	Point of Curvature	SE	Superelevation
CTH	County Trunk Highway	PI	Point of Intersection	SL or S/L	Survey Line
CR.	Creek	PRC	Point of Reverse Curvature	SV	Septic Vent
CY or CU YD	Cubic Yard	PT	Point of Tangency	T	Tangent
CP	Culvert Pipe	POC	Point on Curve	TEL	Telephone
C & G	Curb and Gutter	PVC	Polyvinyl Chloride	TEMP	Temporary
D	Degree of Curve	PCC	Portland Cement Concrete	TI	Temporary Interest
DHV	Design Hour Volume	LB	Pound	t	Ton
DIA	Diameter	PSI	Pounds Per Square Inch	T or TN	Town
E	East	PE	Private Entrance	TRANS	Transition
X	East Grid Coordinate	R	Radius	TL OR T/L	Transit Line
ELEC	Electric	RR	Railroad	T	Trucks (percent of)
EL OR ELEV	Elevation	RL OR R/L	Reference Line	TYP	Typical
ESALS	Equivalent Single Ac=xle Loads	RP	Reference Point	UNCL	Unclassified
EBS	Excavation Below Subgrade	RCCP	Reinforced Concrete Culvert Pipe	UG	Underground Cable
FF	Face to Face	REQD	Required	USH	United States Highway
FE	Field Entrance	RES	Residence or Residential	VAR	Variable
F	Fill	RW	Retaining Wall	V	Velocity or Design Speed
FG	Finished Grade	RT	Right	VERT	Vertical
FL or F/L	Flow Line	RHF	Right–Hand Forward	VC	Vertical Curve
FT	Foot	R/W	Right–of–Way	VOL	Volume
FTG	Footing	R	River	WM	Water Main
GN	Grid North	RD	Road	WV	Water Valve
HT	Height	RDWY	Roadway	W	West
CWT	Hundredweight	SALV	Salvaged	WB	Westbound
HYD	Hydrant	SAN S	Sanitary Sewer	YD	Yard
INL	inlet				
ID	Inside Diameter				
INV	Invert				
IP	Iron Pipe or Pin				
IRS	Iron Rod Set				



TYPICAL EXISTING SECTION



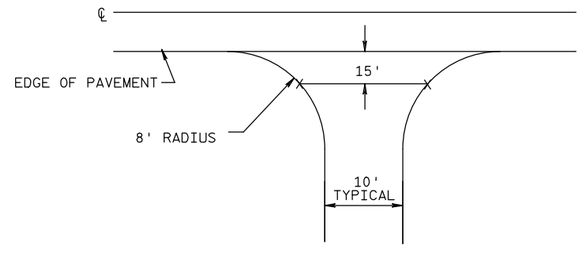
FIELD ENTRANCE - TYPICAL CROSS SECTION



TYPICAL FINISHED SECTION

STA. 12+00 - 14+00

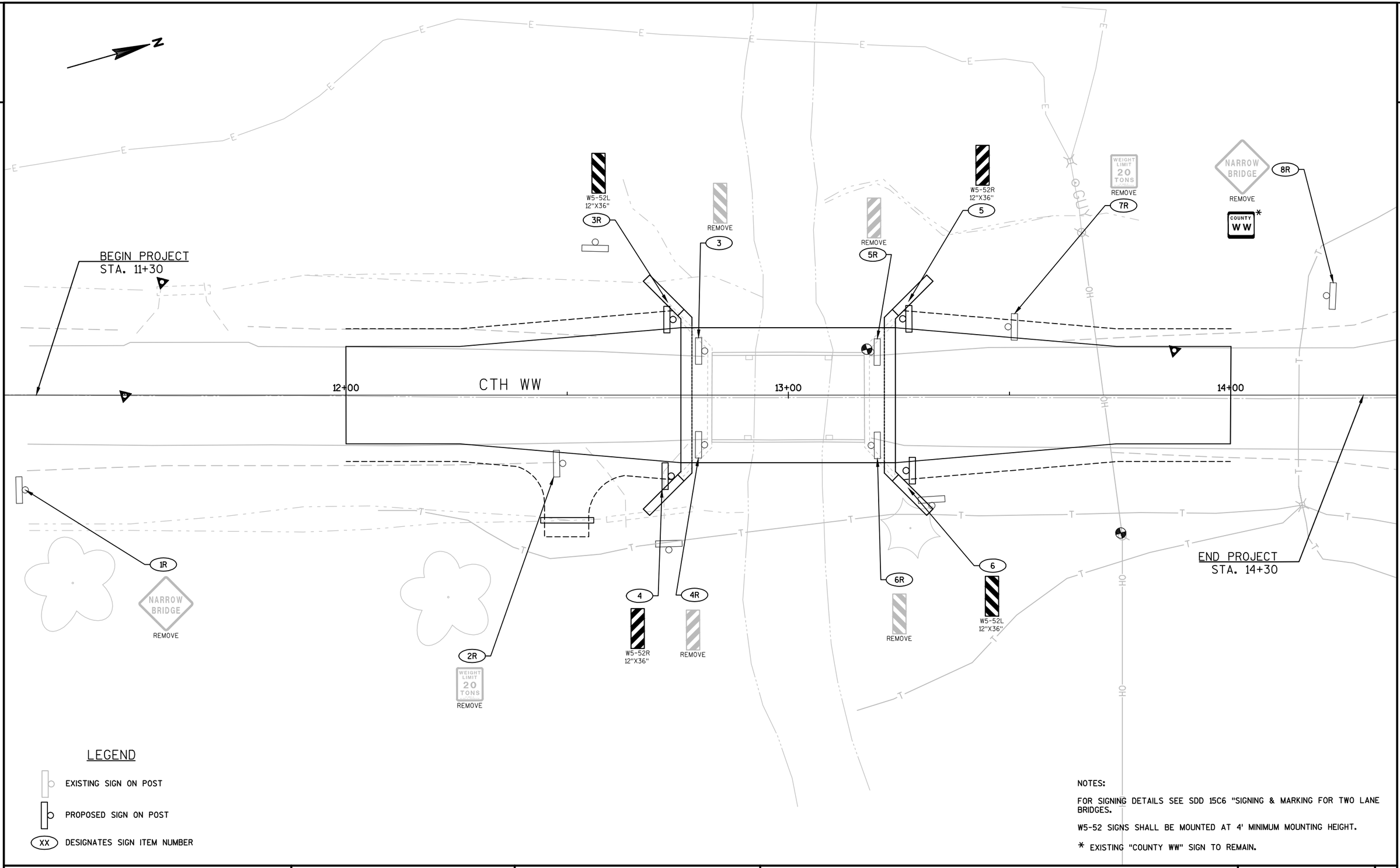
* ASPHALT TO TAPER FROM BRIDGE WIDTH TO 22' AT 50' FROM BRIDGE ENDS.



TYPICAL FIELD ENTRANCE DETAIL

PROJECT NO:5539-00-70	HWY: CTH WW	COUNTY: VERNON	TYPICAL SECTIONS	SHEET 3	E

2



LEGEND

- EXISTING SIGN ON POST
- PROPOSED SIGN ON POST
- XX DESIGNATES SIGN ITEM NUMBER

NOTES:

FOR SIGNING DETAILS SEE SDD 15C6 "SIGNING & MARKING FOR TWO LANE BRIDGES.

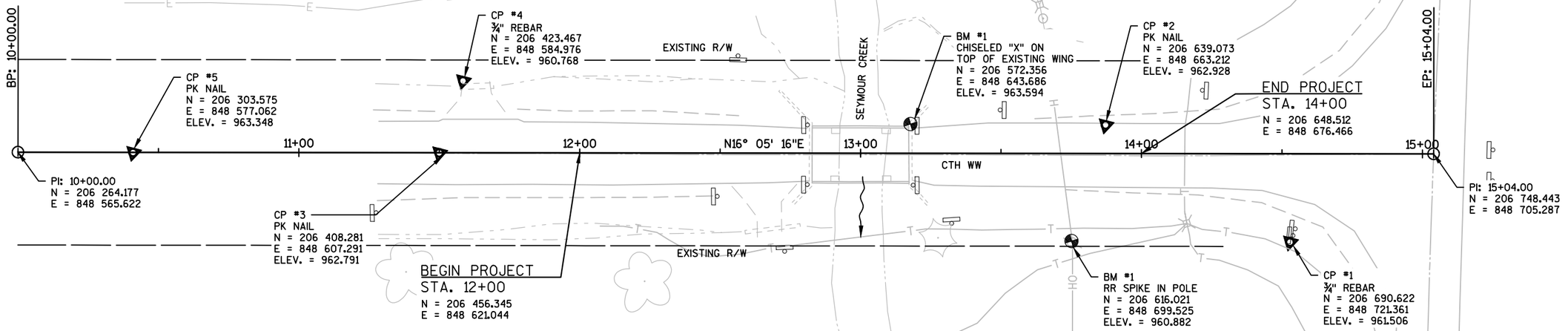
W5-52 SIGNS SHALL BE MOUNTED AT 4' MINIMUM MOUNTING HEIGHT.

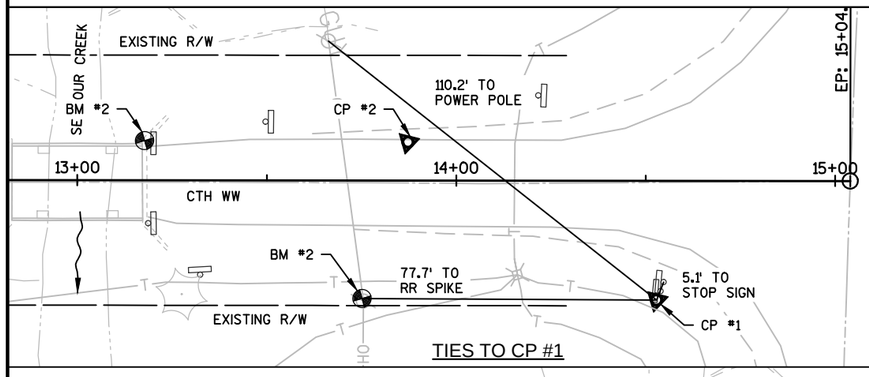
* EXISTING "COUNTY WW" SIGN TO REMAIN.

PROJECT NO: 5539-00-70	HWY: CTH WW	COUNTY: VERNON	SIGNING	SHEET 4	E

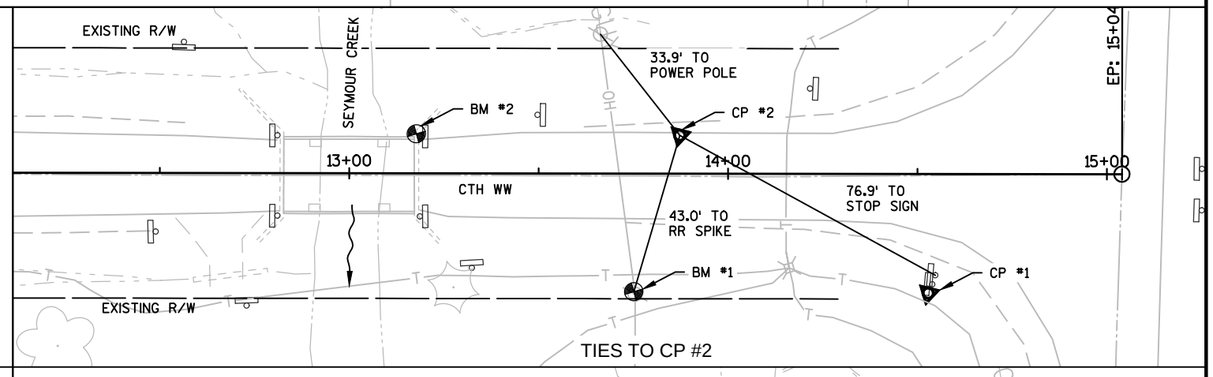
FILE NAME : G:\00-PROJECT FILES\2016\16150 CTH WW, VERNON, SEYMOUR CREEK BRIDGE B-62-0995\CAD\SHEETSPLAN\023201_PS.DWG
LAYOUT NAME - ****
PLOT DATE : 4/26/2018 1:50 PM PLOT BY : LUKE C. SANDER PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 42

2





TIES TO CP #1



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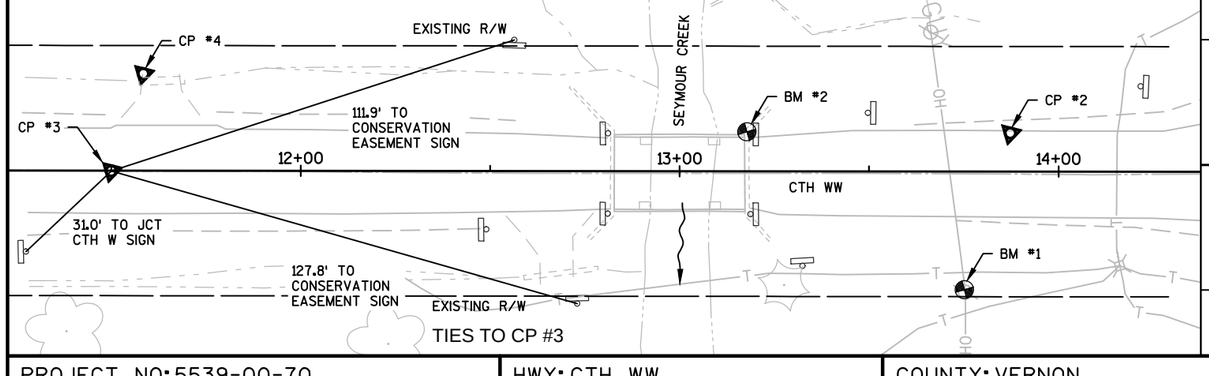
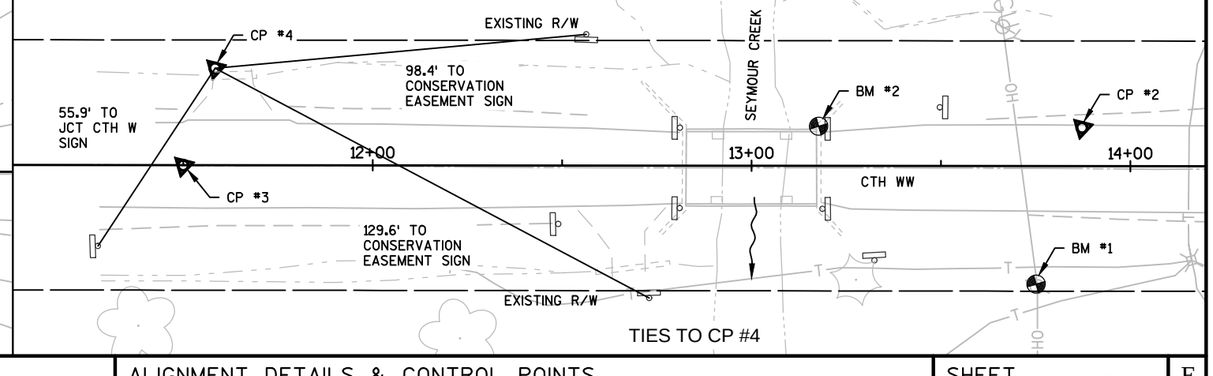


TIES TO CP #3



TIES TO CP #4

PROJECT NO: 5539-00-70	HWY: CTH WW	COUNTY: VERNON	ALIGNMENT DETAILS & CONTROL POINTS	SHEET 5	E

FILE NAME : G:\00-PROJECT FILES\2016\16150 CTH WW, VERNON, SEYMOUR CREEK BRIDGE B-62-0995\CAD\SHEETSPLAN\027201_AD.DWG
LAYOUT NAME - ****
PLOT DATE : 4/26/2018 1:58 PM
PLOT BY : LUKE C. SANDER
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

5539-00-70

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	1.000	1.000
0004	203.0600.S	Removing Old Structure Over Waterway With Minimal Debris (station) 01. 13+00	LS	1.000	1.000
0006	205.0100	Excavation Common **P**	CY	200.000	200.000
0008	206.1000	Excavation for Structures Bridges (structure) 01. B-62-0257	LS	1.000	1.000
0010	210.1500	Backfill Structure Type A	TON	300.000	300.000
0012	213.0100	Finishing Roadway (project) 01. 5539-00-70	EACH	1.000	1.000
0014	305.0110	Base Aggregate Dense 3/4-Inch	TON	43.000	43.000
0016	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	270.000	270.000
0018	455.0605	Tack Coat	GAL	22.000	22.000
0020	465.0105	Asphaltic Surface	TON	95.000	95.000
0022	502.0100	Concrete Masonry Bridges	CY	180.000	180.000
0024	502.3200	Protective Surface Treatment	SY	220.000	220.000
0026	505.0400	Bar Steel Reinforcement HS Structures	LB	4,400.000	4,400.000
0028	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	25,920.000	25,920.000
0030	513.4061	Railing Tubular Type M 01. B-62-0257	LF	102.000	102.000
0032	516.0500	Rubberized Membrane Waterproofing	SY	14.000	14.000
0034	521.1015	Apron Endwalls for Culvert Pipe Steel 15-Inch	EACH	2.000	2.000
0036	521.3115	Culvert Pipe Corrugated Steel 15-Inch	LF	12.000	12.000
0038	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	455.000	455.000
0040	606.0300	Riprap Heavy	CY	159.000	159.000
0042	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	150.000	150.000
0044	618.0100	Maintenance And Repair of Haul Roads (project) 01. 5539-00-70	EACH	1.000	1.000
0046	619.1000	Mobilization	EACH	1.000	1.000
0048	624.0100	Water	MGAL	15.000	15.000
0050	625.0500	Salvaged Topsoil	SY	240.000	240.000
0052	627.0200	Mulching	SY	390.000	390.000
0054	628.1504	Silt Fence	LF	240.000	240.000
0056	628.1520	Silt Fence Maintenance	LF	240.000	240.000
0058	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0060	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0062	628.6005	Turbidity Barriers	SY	135.000	135.000
0064	629.0210	Fertilizer Type B	CWT	0.450	0.450
0066	630.0120	Seeding Mixture No. 20	LB	16.000	16.000
0068	630.0200	Seeding Temporary	LB	16.000	16.000
0070	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	4.000	4.000
0072	637.2230	Signs Type II Reflective F	SF	12.000	12.000
0074	638.2602	Removing Signs Type II	EACH	4.000	4.000

5539-00-70

Line	Item	Item Description	Unit	Total	Qty
0076	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0078	642.5001	Field Office Type B	EACH	1.000	1.000
0080	643.0420	Traffic Control Barricades Type III	DAY	1,278.000	1,278.000
0082	643.0705	Traffic Control Warning Lights Type A	DAY	1,988.000	1,988.000
0084	643.0900	Traffic Control Signs	DAY	994.000	994.000
0086	643.5000	Traffic Control	EACH	1.000	1.000
0088	645.0111	Geotextile Type DF Schedule A	SY	92.000	92.000
0090	645.0120	Geotextile Type HR	SY	284.000	284.000
0092	646.1020	Marking Line Epoxy 4-Inch	LF	800.000	800.000
0094	650.4500	Construction Staking Subgrade	LF	152.000	152.000
0096	650.5000	Construction Staking Base	LF	152.000	152.000
0098	650.6500	Construction Staking Structure Layout (structure) 01. B-62-0257	LS	1.000	1.000
0100	650.9910	Construction Staking Supplemental Control (project) 01. 5539-00-70	LS	1.000	1.000
0102	650.9920	Construction Staking Slope Stakes	LF	152.000	152.000
0104	690.0150	Sawing Asphalt	LF	44.000	44.000
0106	715.0502	Incentive Strength Concrete Structures	DOL	1,080.000	1,080.000

3

REMOVING SMALL PIPE CULVERTS

STATION	LOCATION	203.0100 (EACH)	REMARKS
12+50	RT	1	15" CMCP
	TOTAL	1	

EARTHWORK SUMMARY

STATION - STATION	LOCATION	**P** 205.0100 EXCAVATION COMMON (1) (CY)	UNEXPANDED FILL (CY)	EXPANDED FILL (2) (25%) (CY)	MASS ORDINATE +/- (3) (CY)
12+00.00 - 12+75.75	MAINLINE	103	67	87	16
13+24.25 - 14+00.00	MAINLINE	97	0	0	97
	TOTALS	200	67	87	113

NOTES:
1) EBS IS NOT INCLUDED IN DIVISIONS
2) EXPANDED FILL FACTOR = 1.30; EXPANDED FILL = UNEXPANDED FILL * 1.30
P - PAY PLAN QUANTITY

BASE AGGREGATE DENSE

STATION - STATION	LOCATION	305.0120 1 1/4-INCH BASE (TON)	305.0110 3/4-INCH SHLD (TON)	624.0100 WATER* (MGAL)
12+00.00 - 12+75.75	MAINLINE	135	21	3
13+24.25 - 14+00.00	MAINLINE	135	19	3
12+50	FE, RT	—	3	---
	TOTALS	270	43	6

*ADDITIONAL QUANTITY INCLUDED WITH EROSION CONTROL ITEMS

ASPHALTIC ITEMS

STATION - STATION	LOCATION	465.0105 ASPHALTIC SURFACE (TON)	455.0600 TACK COAT (GAL)
12+00.00 - 12+75.75	MAINLINE	48	11
13+24.25 - 14+00.00	MAINLINE	47	11
	TOTALS	95	22

PIPE CULVERTS

STATION	LOCATION	521.1015 APRON ENDWALLS FOR CULVERT PIPE STEEL 15-INCH (EACH)	521.3115 CULVERT PIPE CORRUGATED STEEL 15-INCH (LF)
12+50	RT	2	12
	TOTAL	2	12

MAINTENANCE AND REPAIR OF HAUL ROADS

LOCATION	618.0100** ID 5539-00-70 (EACH)
PROJECT	1
TOTAL	1

**CATEGORY 0030 ITEM

FINISHING ITEMS

STATION - STATION	LOCATION	625.0500 SALVAGED TOPSOIL (SY)	627.0200 MULCHING (SY)	629.0210 FERTILIZER TYPE B (CWT)	630.0200 SEEDING TEMPORARY (LB)	630.0120 SEEDING, MIXTURE NO. 20 (LB)	624.0100 WATER* (MGAL)
12+00.00 - 12+75.75	MAINLINE	115	180	0.20	7	7	4
13+24.25 - 14+00.00	MAINLINE	100	175	0.20	6	6	4
	UNDISTRIBUTED	25	35	0.05	3	3	1
	TOTALS	240	390	0.45	16	16	9

*ADDITIONAL QUANTITY INCLUDED WITH BASE AGGREGATE ITEMS

MOBILIZATIONS EROSION CONTROL

LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL (EACH)	629.1905 MOBILIZATIONS EMERGENCY EROSION CONTROL (EACH)
ID 5539-00-70	2	2
TOTALS	2	2

SILT FENCE

STATION - STATION	LOCATION	628.1504 SILT FENCE (LF)	628.1520 SILT FENCE MAINTENANCE (LF)
13+24.25 - 14+00.00	RT	95	95
13+24.25 - 14+00.00	LT	95	95
	UNDISTRIBUTED	50	50
	TOTALS	240	240

TURBIDITY BARRIERS

LOCATION	628.6005 (SY)
SOUTH ABUT	65
NORTH ABUT	70
TOTALS	135

PERMANENT SIGNING

STATION	LOCATION	SIGN NUMBER	SIGN CODE	SIZE	634.0612 POSTS WOOD 4X6-INCH X 12 FT (EACH)	637.2230 SIGNS TYPE II REFLECTIVE F (SF)	SIGN MESSAGE
12+75	LT	3	W5-52L	12 "X 36"	1	3	TIGER STRIPE MARKER
12+75	RT	4	W5-52R	12 "X 36"	1	3	TIGER STRIPE MARKER
13+25	LT	5	W5-52L	12 "X 36"	1	3	TIGER STRIPE MARKER
13+25	RT	6	W5-52R	12 "X 36"	1	3	TIGER STRIPE MARKER
				TOTAL	4	12	

ALL ITEMS CATEGORY 0010 UNLESS NOTED OTHERWISE.

REMOVING SIGNS & SUPPORTS

STATION	SIGN NUMBER	SIGN CODE	638.2602 REMOVING SIGNS TYPE II (EACH)	638.3000 REMOVING SMALL SIGN SUPPORTS (EACH)	COMMENTS
11+30, RT	1R	W5-2	—	---	TO BE REMOVED BY COUNTY
12+49, RT	2R	R12-1	—	---	TO BE REMOVED BY COUNTY
12+81, LT	3R	W5-52L	1	1	EXISTING TIGER STRIPE MARKER
12+81, RT	4R	W5-52R	1	1	EXISTING TIGER STRIPE MARKER
13+19, LT	5R	W5-52L	1	1	EXISTING TIGER STRIPE MARKER
13+19, RT	6R	W5-52R	1	1	EXISTING TIGER STRIPE MARKER
13+50, LT	7R	R12-1	—	---	TO BE REMOVED BY COUNTY
12+22, LT	8R	W5-2	—	---	TO BE REMOVED BY COUNTY
		TOTAL	4	4	

TRAFFIC CONTROL ITEMS

LOCATION	DURATION	643.0420 TRAFFIC CONTROL BARRICADES, TYPE III (NO.)	643.0420 (DAYS)	643.0705 TRAFFIC CONTROL WARNING LIGHTS, TYPE A (NO.)	643.0705 (DAYS)	643.0900 TRAFFIC CONTROL SIGNS (NO.)	643.0900 (DAYS)	643.5000 TRAFFIC CONTROL EACH
AS DIRECTED BY ENGINEER	71	18	1278	28	1988	14	994	—
CTH WW	---	---	---	---	---	---	---	1
		18	1278	28	1988	14	994	1

PLACE TRAFFIC CONTROL IN ACCORDANCE WITH SDD 15C2

MARKING LINE EPOXY 4-INCH

STATION - STATION	LOCATION	646.1020 (LF)	COMMENTS
12+00 - 14+00	CENTERLINE	400	DBL YELLOW
12+00 - 14+00	PAVEMENT EDGE	400	WHITE
	TOTAL	800	

STAKING ITEMS

CATEGORY	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE (LF)	650.5000 CONSTRUCTION STAKING BASE (LF)	650.6500 CONSTRUCTION STAKING STRUCTURE LAYOUT (B-62-0257) (LS)	650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (5539-00-70) (LS)	650.9920 CONSTRUCTION STAKING SLOPE STAKES (LF)
0010	12+00 - 14+00	152	152	—	1	152
0020	B-62-0257	—	---	1	---	---
	TOTALS	152	152	0	1	152

SAWING EXISTING PAVEMENT

STATION	LOCATION	690.0150 (L.F.)
12+00	MAINLINE	22
14+00	MAINLINE	22
	TOTAL	44

LEGEND

XXXX SAWCUT, MATCH EXISTING
FLOW ARROW
STREAM EDGE
WETLANDS
SILT FENCE
TURBIDITY BARRIER

FRANK KABLAU AND JOANN BLONIEN

REMOVE EXISTING STRUCTURE B-62-0995
STA. 13+00, SINGLE SPAN FLAT SLAB BRIDGE
36-FT OVERALL LENGTH AND 19-FT CLEAR
ROADWAY WIDTH

IRENE GORNIK

SEYMOUR CREEK

CTH W

VERNON ELECTRIC COOPERATIVE

SLOPE INTERCEPT, TYP.

PROPOSED ℄ CTH WW

RIPRAP HEAVY

CENTURYLINK

CAUTION

EXISTING R/W

CP #4

BM #2

11+00 12+00 CTH WW (ASPHALT) 13+00 N16° 05' 16.39"E CP #2 14+00 15+00

CP #5

CP #3

EXISTING R/W

STA. 12+50, RT
CONSTRUCT F.E.

CENTURYLINK

BM #1

SLOPE INTERCEPT, TYP.

CP #1

CENTURYLINK

END PROJECT
STA. 14+00
N= 206 648.512
E= 848 676.466

BEGIN PROJECT
STA. 12+00
N= 206 456.345
E= 848 621.044

FRANK KABLAU AND JOANN BLONIEN

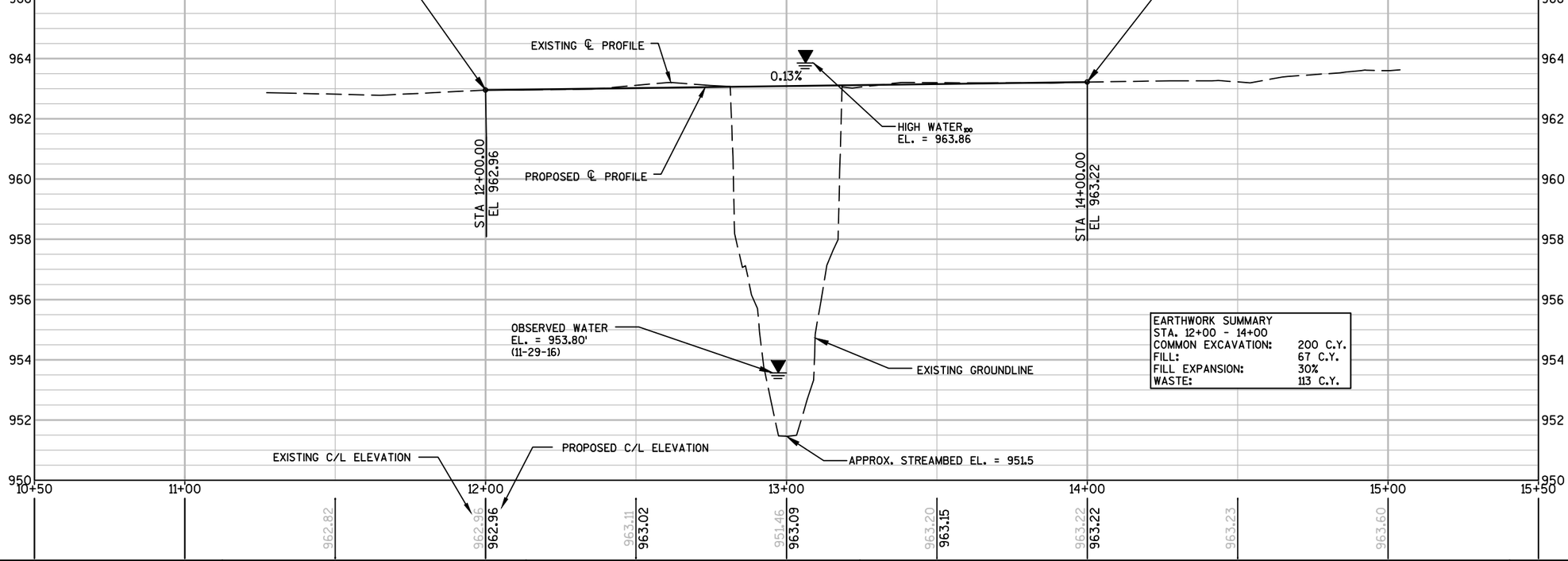
STA. 13+00, STRUCTURE B-62-0257
SINGLE SPAN FLAT CONCRETE SLAB
28-FT CLEAR ROADWAY WIDTH
0.0° SKEW

VERNON ELECTRIC COOPERATIVE

IRENE GORNIK

BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
1	13+75.17	RAILROAD SPIKE SET EAST SIDE OF TELEPHONE POLE, 31.16' RT	960.88
2	13+11.25	CHISELED "X" ON TOP OF EXISTING NORTHEAST WINGWALL, 10.39' LT	963.59



EARTHWORK SUMMARY
STA. 12+00 - 14+00

COMMON EXCAVATION:	200 C.Y.
FILL:	67 C.Y.
FILL EXPANSION:	30%
WASTE:	113 C.Y.

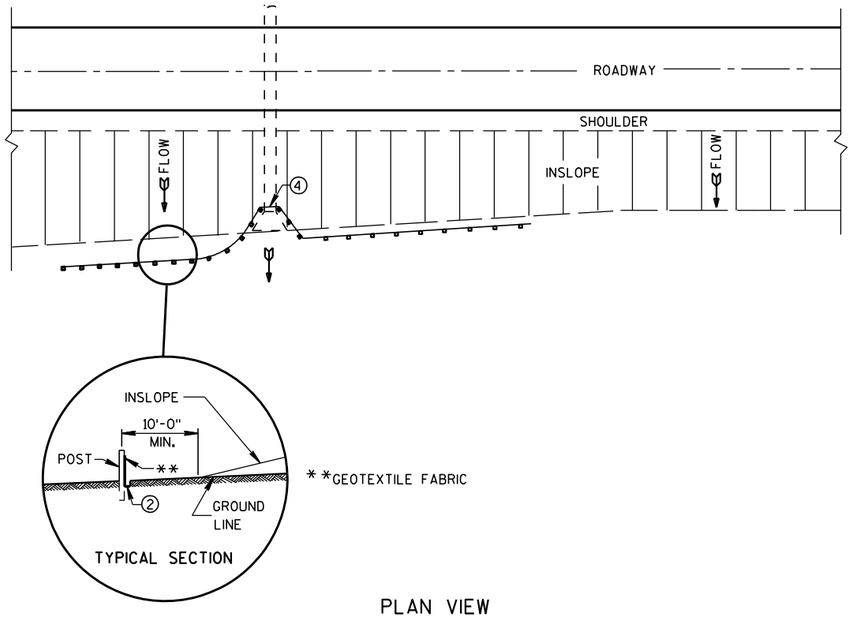
PROJECT NO: 5539-00-70	HWY: CTH WW	COUNTY: VERNON	PLAN AND PROFILE: MAINLINE	SHEET 9 E

FILE NAME : G:\00-PROJECT FILES\2016\16150 CTH WW, VERNON, SEYMOUR CREEK BRIDGE B-62-0995\CAD\SHEETSPLAN\050101_PP.DWG
LAYOUT NAME - ****
PLOT DATE : 6/11/2018 1:58 PM
PLOT BY : LUKE C. SANDER
PLOT NAME :
PLOT SCALE : 1 IN:40 FT

Standard Detail Drawing List

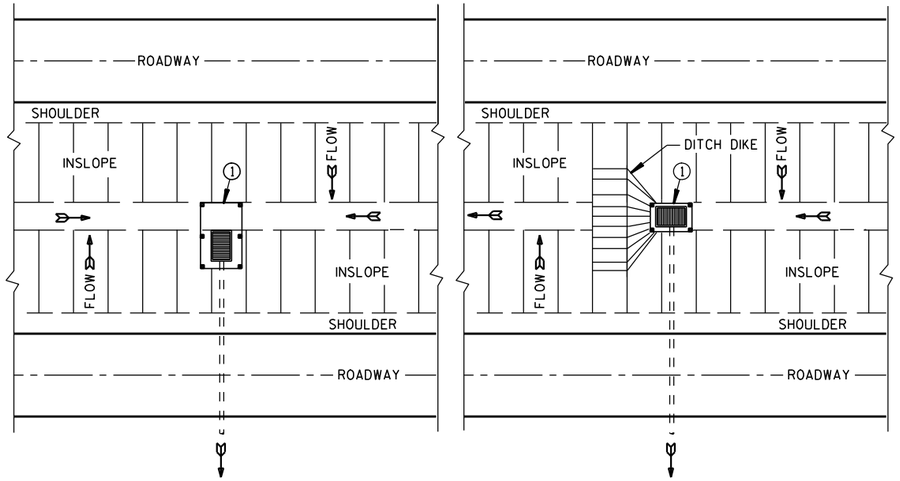
08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
12A03-10	NAME PLATE (STRUCTURES)
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C06-09	SIGNING & MARKING FOR TWO LANE BRIDGES
15C08-18A	LONGITUDINAL MARKING (MAINLINE)

6



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



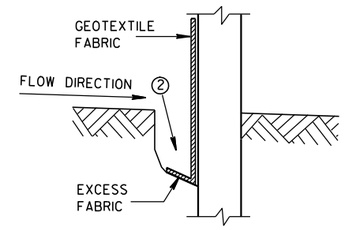
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

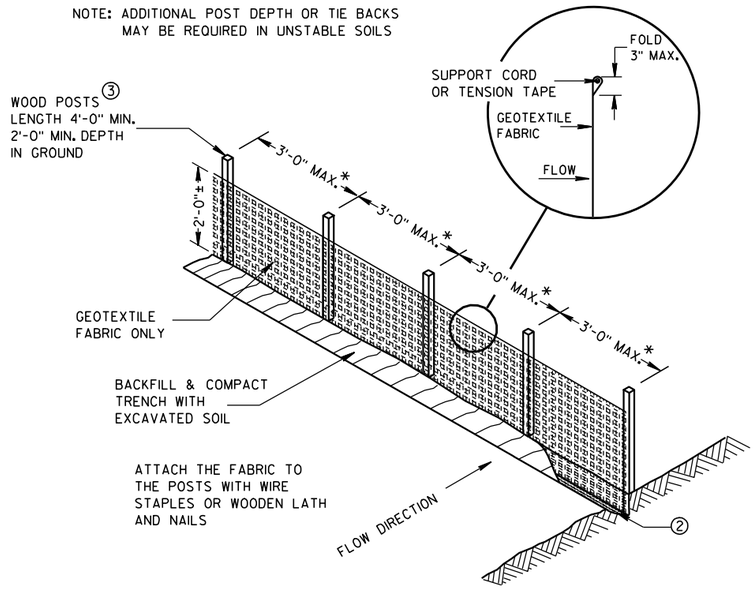
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

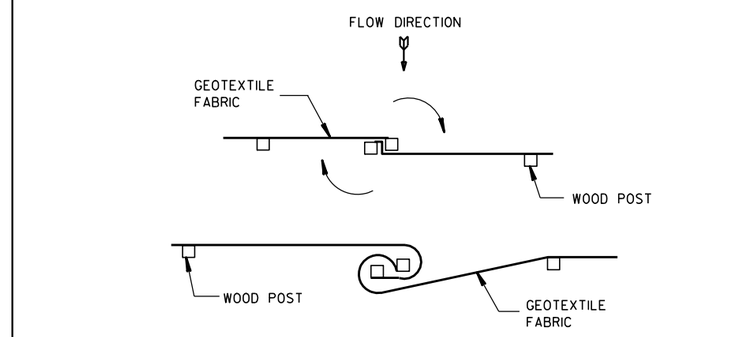


TRENCH DETAIL

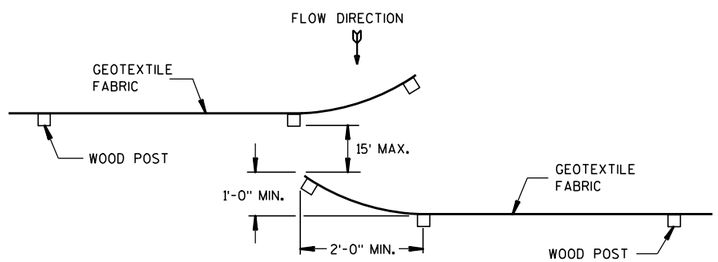


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

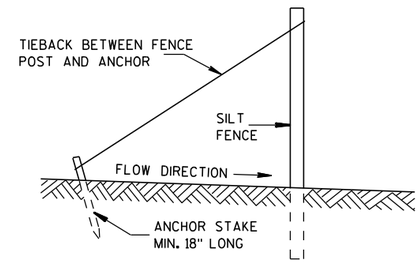


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

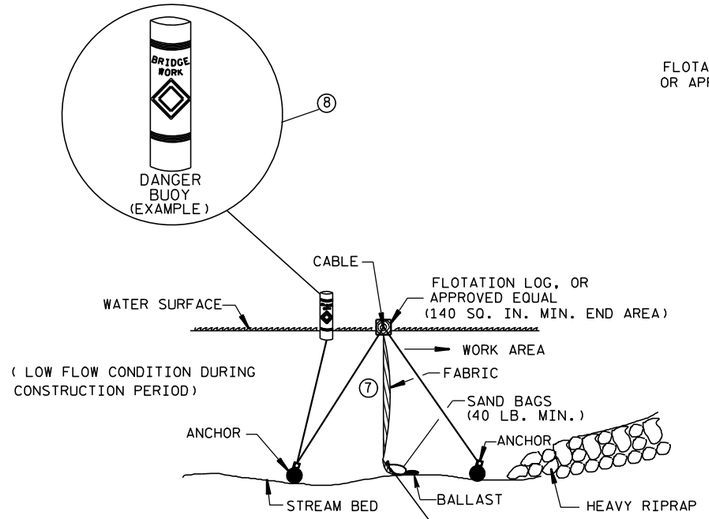
SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

6

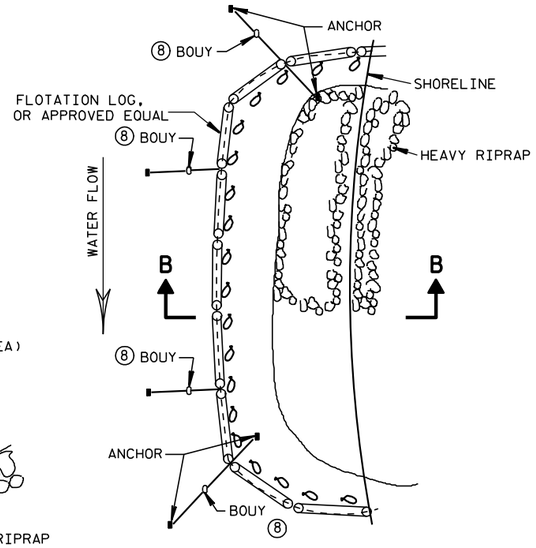
S.D.D. 8 E 9-6

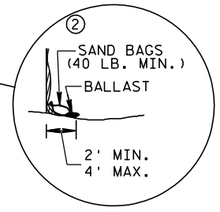
6

S.D.D. 8 E 9-6

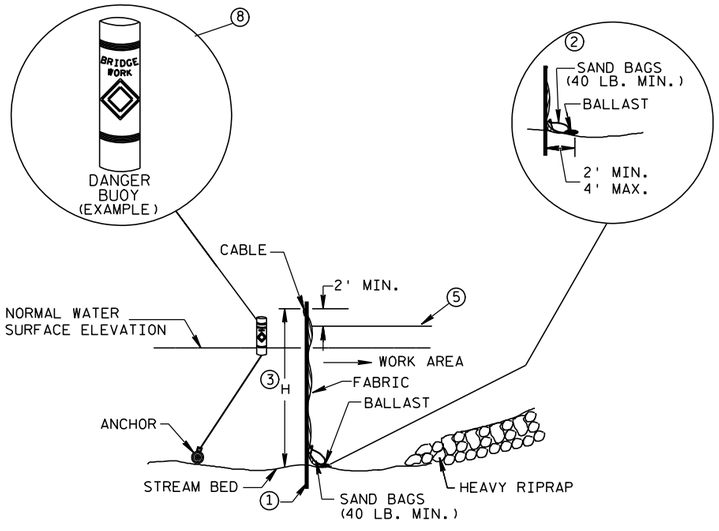


SECTION B-B

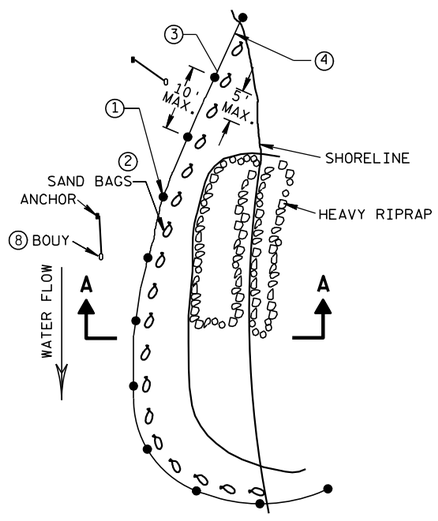




TURBIDITY BARRIER FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6



SECTION A-A



PLAN VIEW

TURBIDITY BARRIER STANDARD POST INSTALLATION

TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.

② SANDBAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.

③ WHEN BARRIER HEIGHT, H, EXCEEDS 8 FT., POST SPACING MAY NEED TO BE DECREASED.

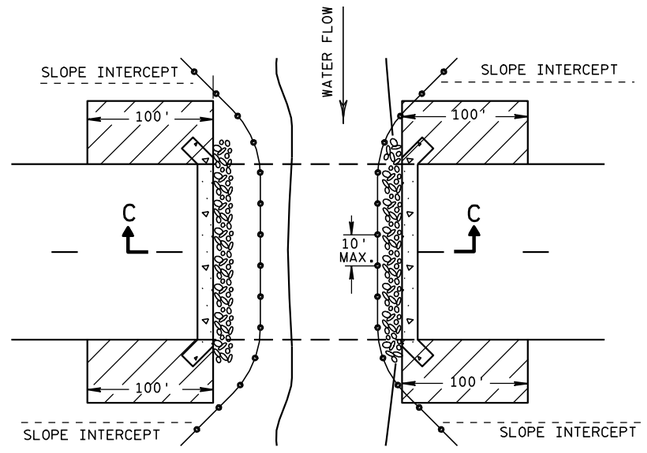
④ IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.

⑤ ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MIMIMUM BARRIER HEIGHT SHALL BE 2'GREATER THAN EITHER THE O2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WICHEVER IS GREATER.

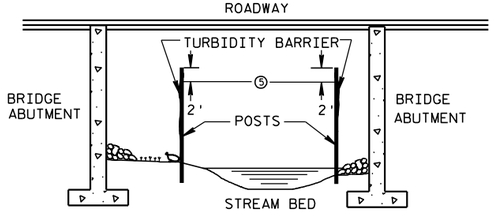
⑥ FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BED ROCK PREVENTS THE INSTALLATION OF POSTS.

⑦ ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.

⑧ USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C-C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

TURBIDITY BARRIER	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
6/04/02 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

6

S.D.D. 8 E 11-2

METAL APRON ENDWALLS

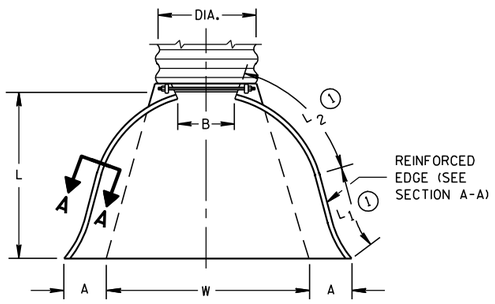
PIPE DIA. (IN.)	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109×	.105×	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109×	.105×	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109×	.105×	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109×	.105×	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109×	.105×	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109×	.105×	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109×	.105×	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

× EXCEPT CENTER PANEL SEE GENERAL NOTES

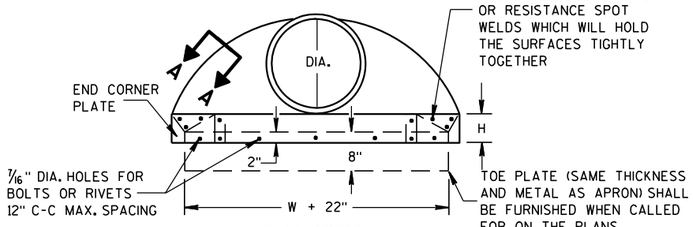
REINFORCED CONCRETE APRON ENDWALLS

PIPE DIA. (IN.)	DIMENSIONS (Inches) T	A	B	C	D	E	G	APPROX. SLOPE
12	2	4	24	48 7/8	72 1/8	24	2	3 to 1
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1
48	5	24	72	26	98	84	5	3 to 1
54	5 1/2	27	65	* 33 1/4 - ** 35	* 98 1/4 - ** 100	90	5 1/2	2 2/5 to 1
60	6	* 30 - ** 35	60	39	99	96	5	2 to 1
66	6 1/2	* 24 - ** 30	* 72 - ** 78	* 21 - ** 27	99	102	5 1/2	2 to 1
72	7	* 24 - ** 36	78	21	99	108	6	2 to 1
78	7 1/2	* 24 - ** 36	78	21	99	114	6 1/2	2 to 1
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1

*MINIMUM
**MAXIMUM



PLAN VIEW



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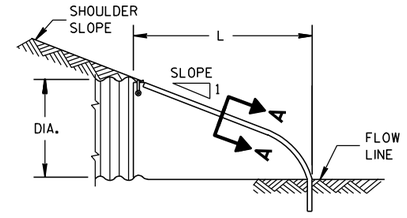
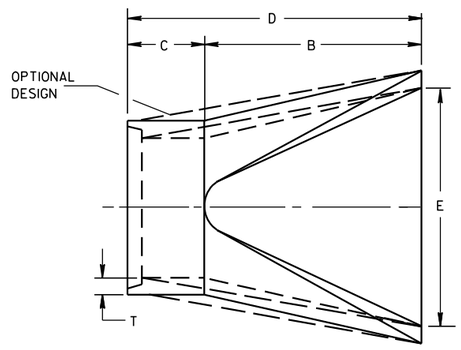
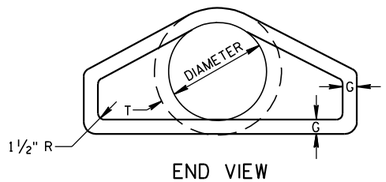


SIDE ELEVATION

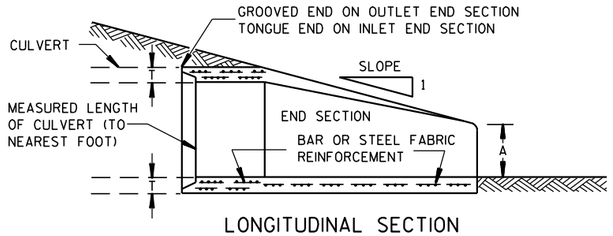
METAL ENDWALLS



PLAN

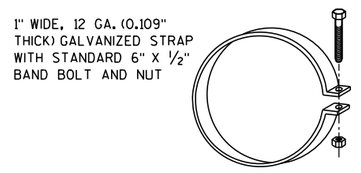


END VIEW



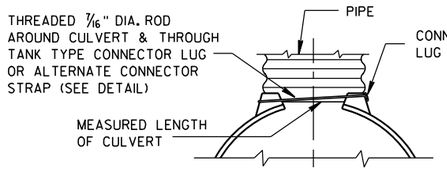
LONGITUDINAL SECTION

CONCRETE ENDWALLS

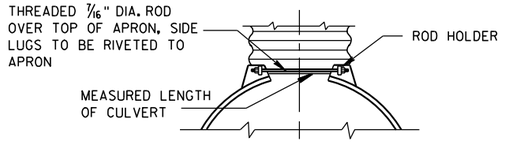


ALTERNATE FOR TYPE 1 CONNECTION

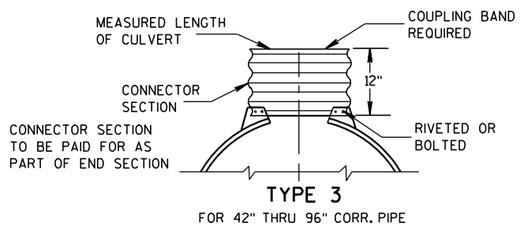
END SECTION CONNECTOR STRAP



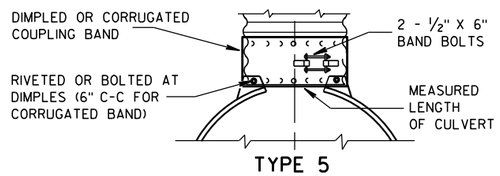
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

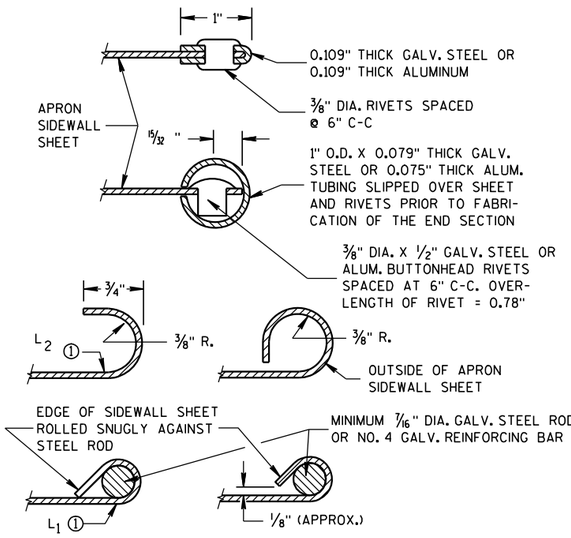
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL. AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

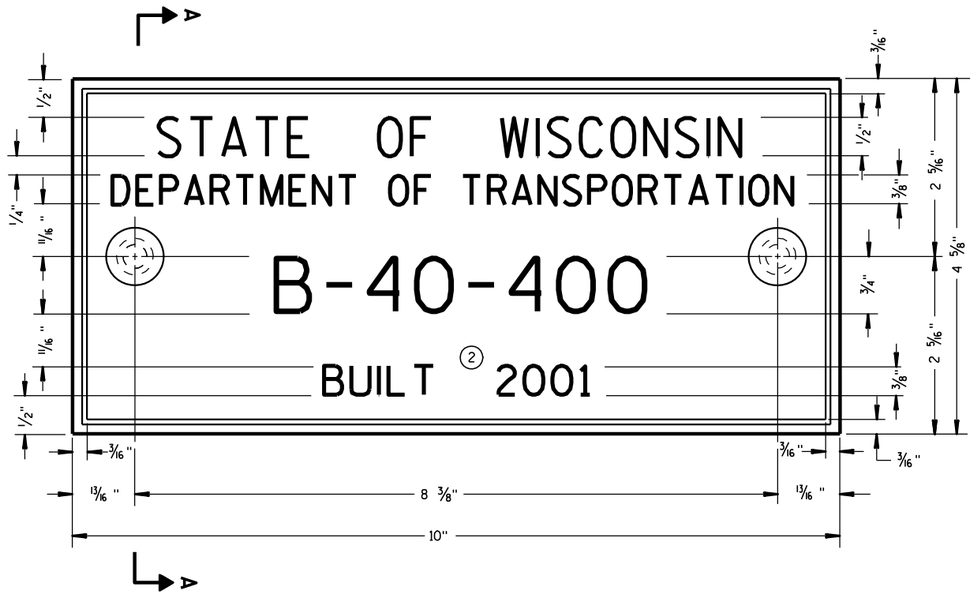
APRON ENDWALLS FOR CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 DATE
/S/ Rory L. Rhinesmith
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 8 F 1-11



TYPICAL NAME PLATE

(BRIDGES, CULVERTS, AND RETAINING WALLS)

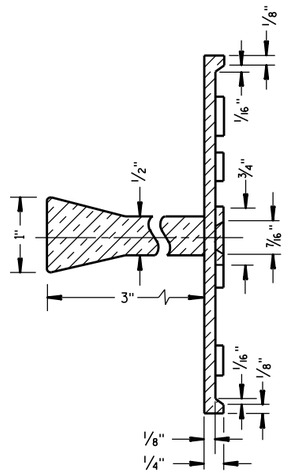
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

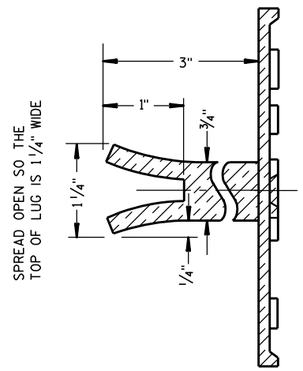
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A



ALTERNATE LUG

FOR MULTI-UNIT STRUCTURES
LINE 3 ABOVE SHALL READ

B = BRIDGE
C = CULVERT
R = RETAINING WALL

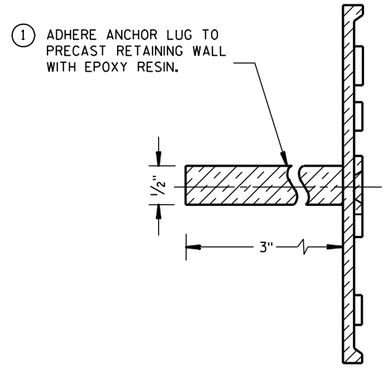
COUNTY NO.

BRIDGE NO.

UNIT NO. FOR MULTIPLE UNIT BRIDGE

B-40-400-1A

NUMBERING DESIGNATION MULTI-UNIT STRUCTURES



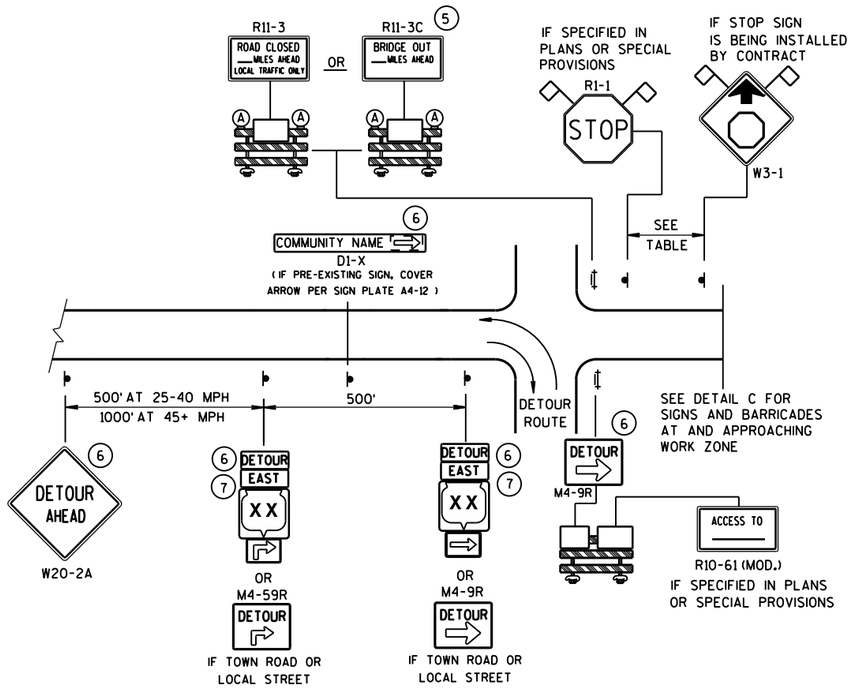
ALTERNATE LUG

(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
3/26/10 DATE	/S/ Scot Beck CHIEF STRUCTURAL DEVELOP ENGINEER
FHWA	

6

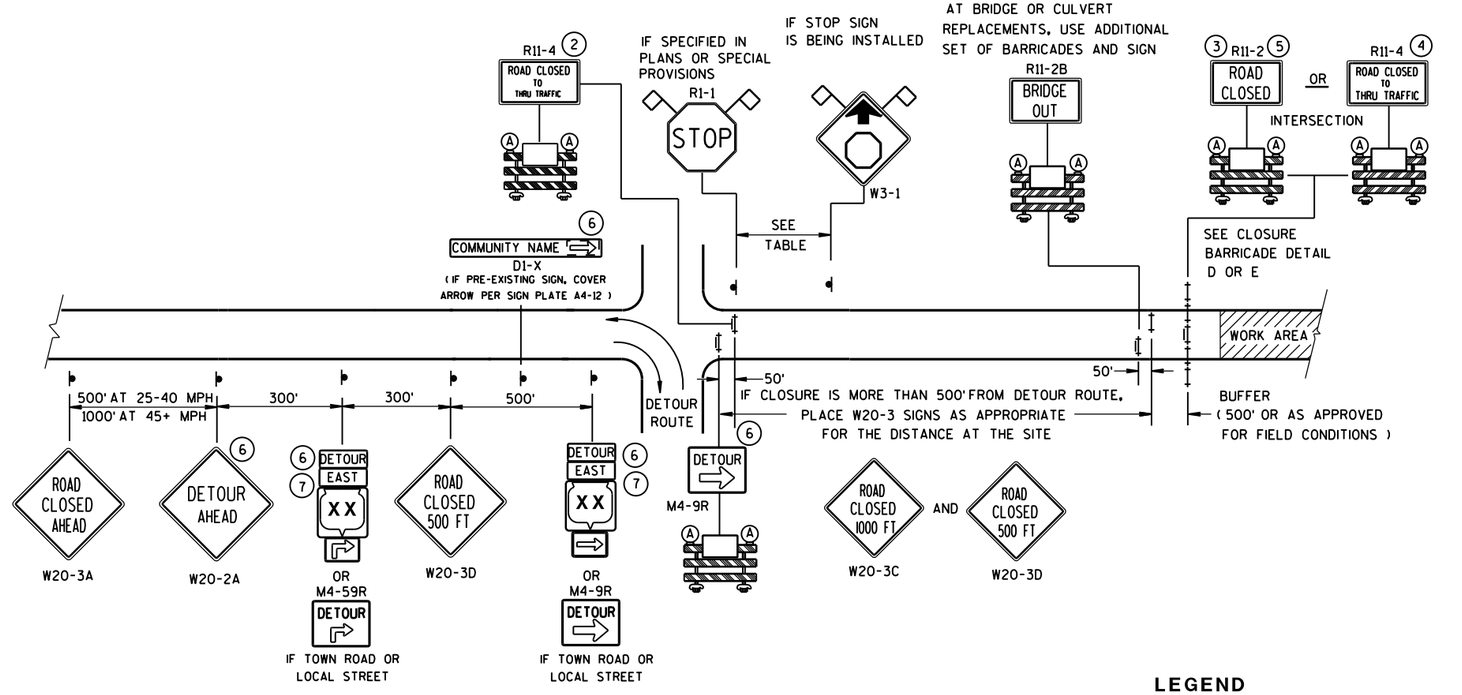
S.D.D. 12 A 3-10



DETAIL A

MAINLINE CLOSURE WITH POSTED DETOUR

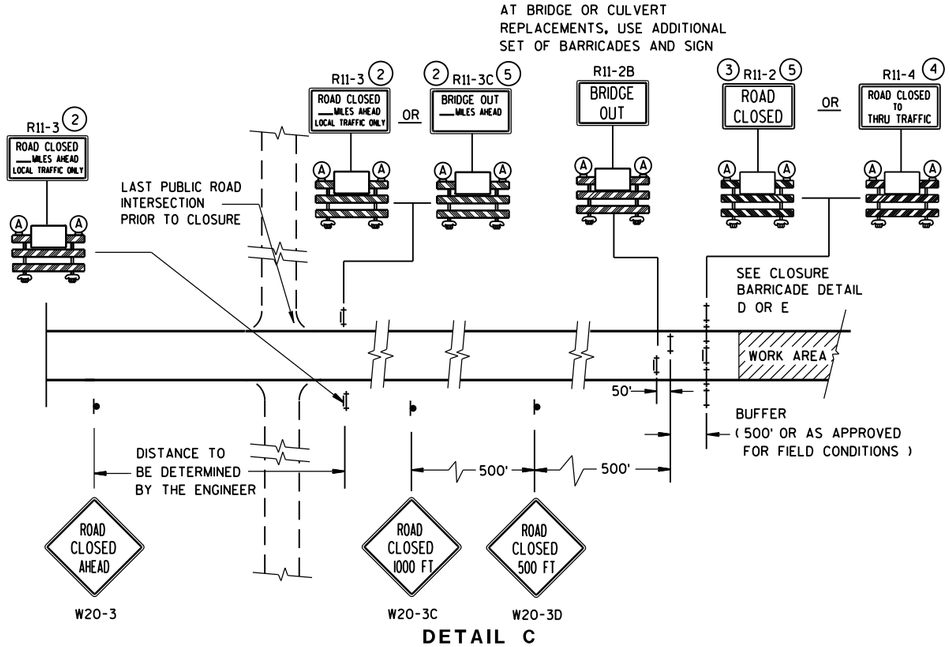
WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B

MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C

MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "**b**" FOR GENERAL NOTES AND FOOTNOTES (1) THROUGH (7)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

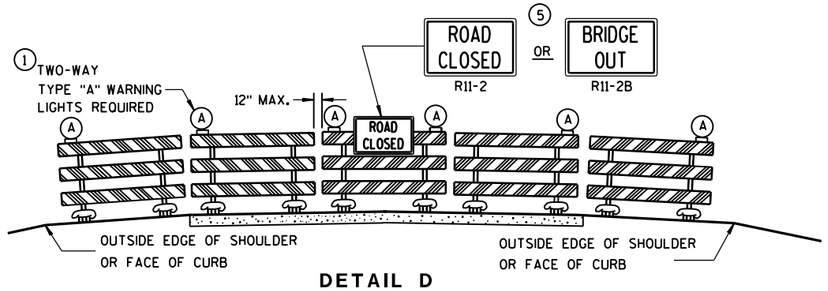
Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA

6

S.D.D. 15 C 2-6a

6

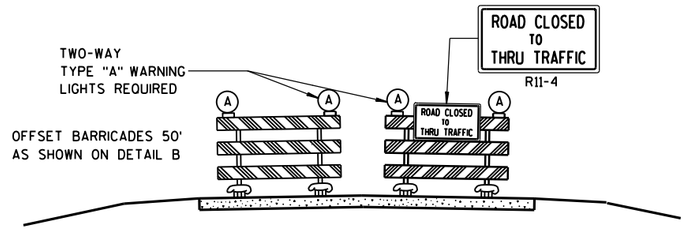
S.D.D. 15 C 2-6a



DETAIL D

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E

LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 — DATE

/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO[illegible] SAFETY ENGI[illegible] — FHWA

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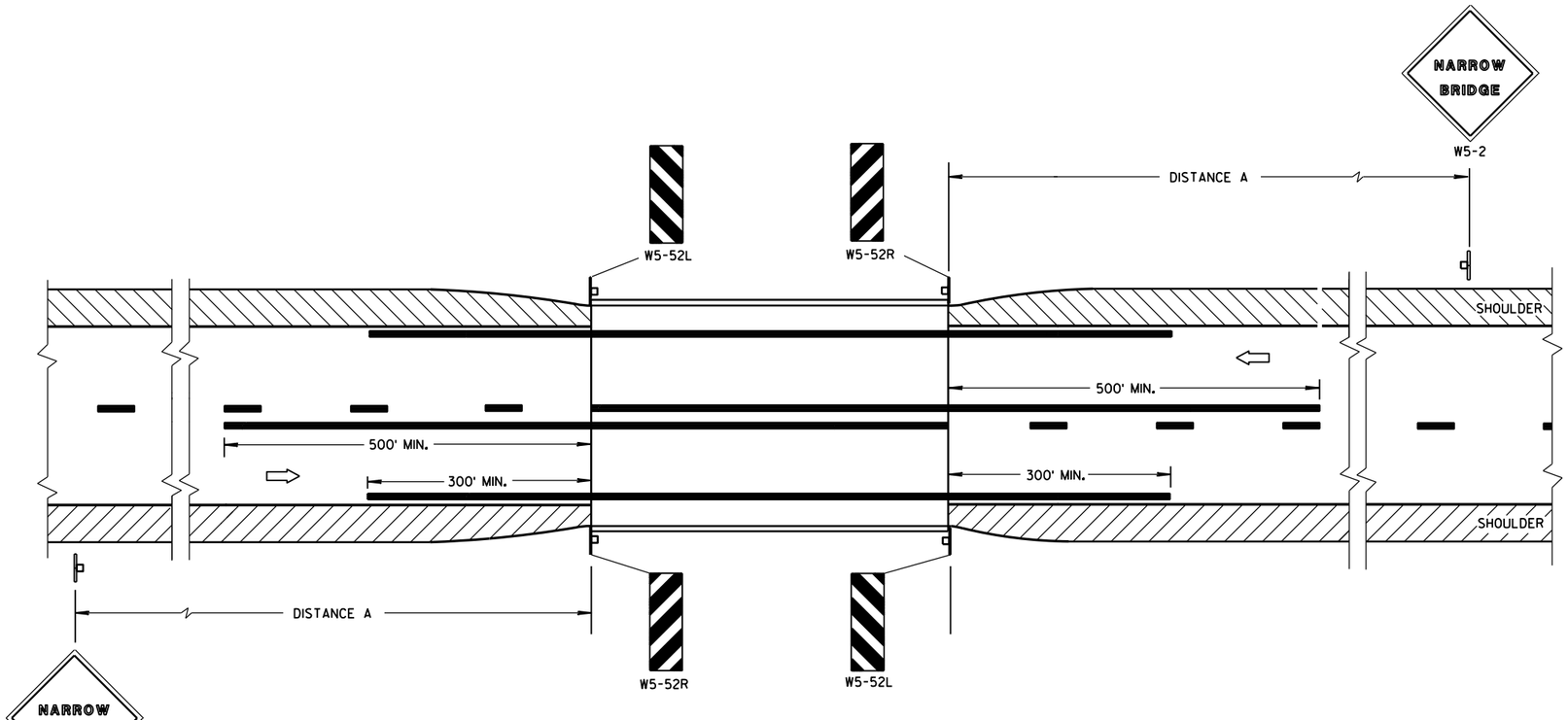
S.D.D. 15 C 2-6b



SITUATION 1

WARRANTING CRITERIA:

BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

GENERAL NOTES

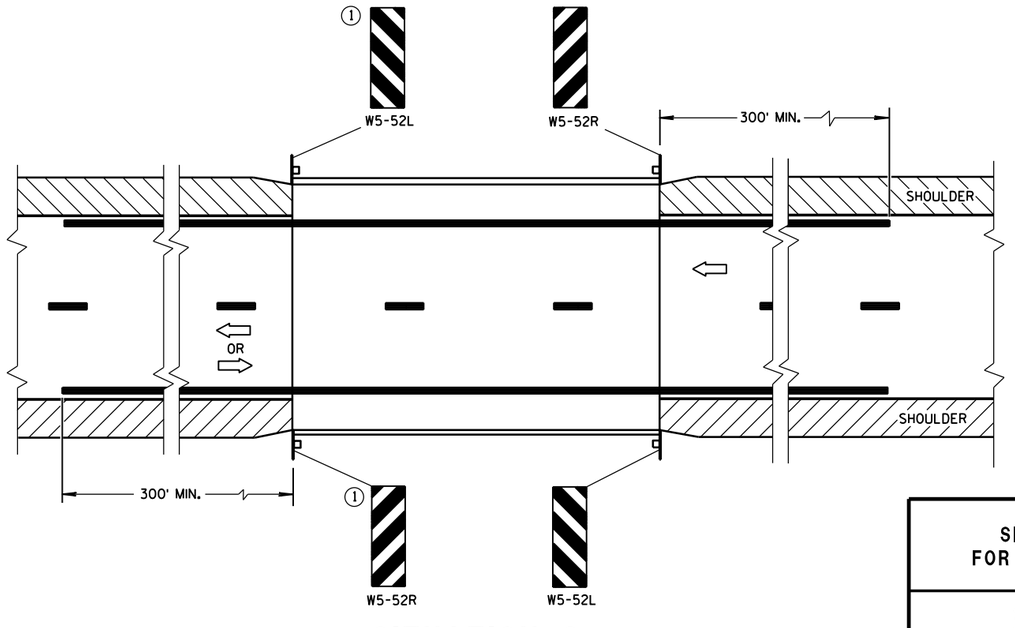
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

(1) OMIT ON ONE-WAY TRAVELLED WAYS.

⇨ DIRECTION OF TRAFFIC



SITUATION 2

WARRANTING CRITERIA:

1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

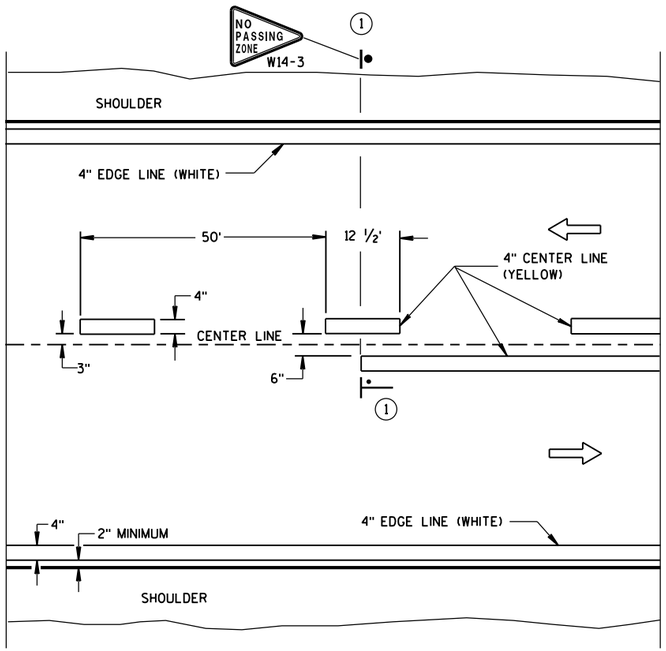
SIGNING & MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

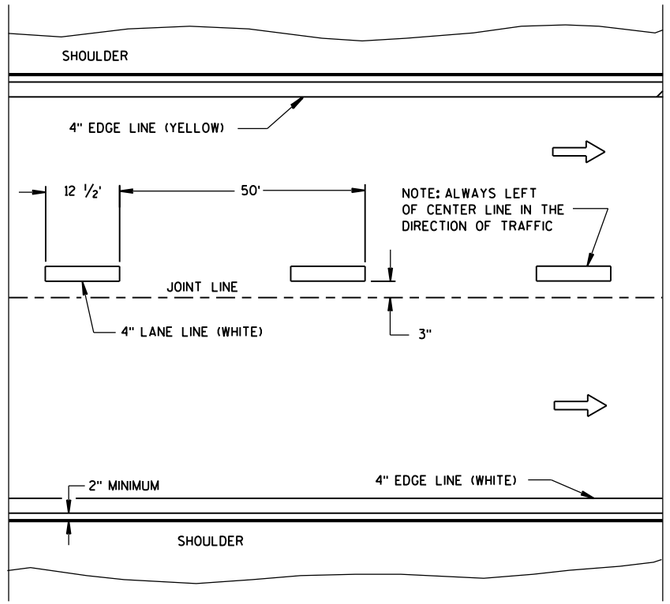
APPROVED
June 2017 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MAR[illegible]EER
FHWA

6

S.D.D. 15 C 6-9



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

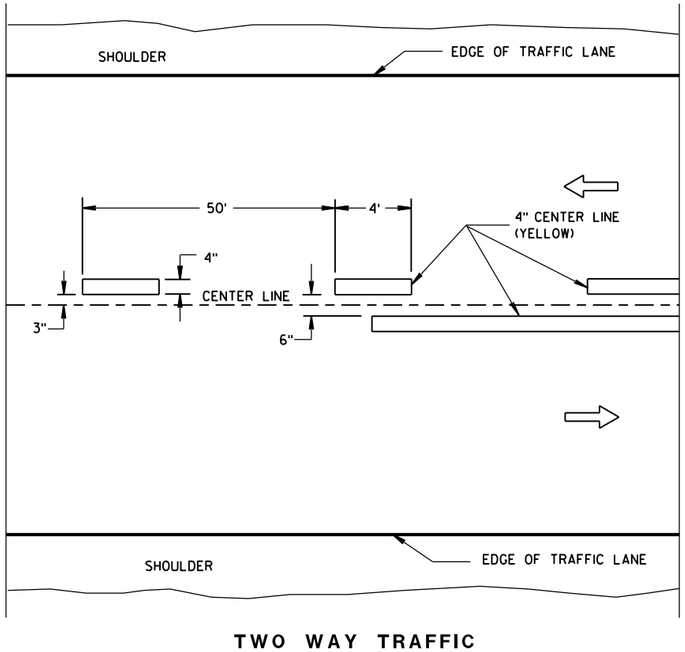
NOTE

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

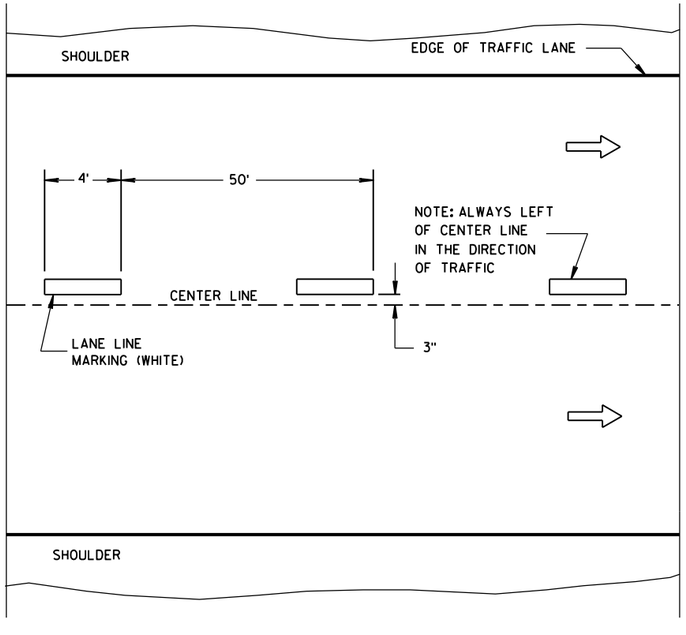
LEGEND

"T" MARKING

POST MOUNTED SIGN



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MAR[illegible]INEER
FHWA

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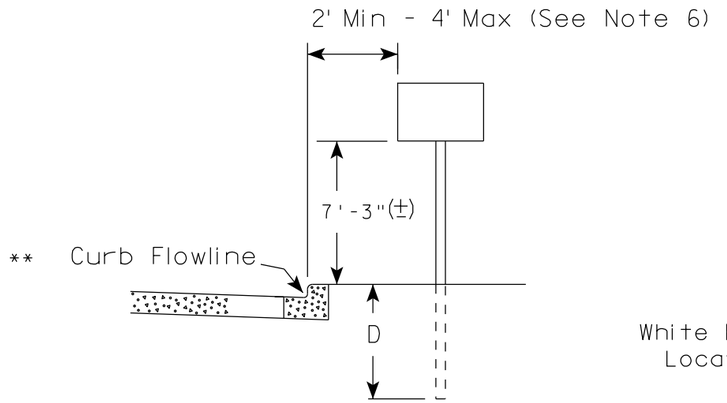
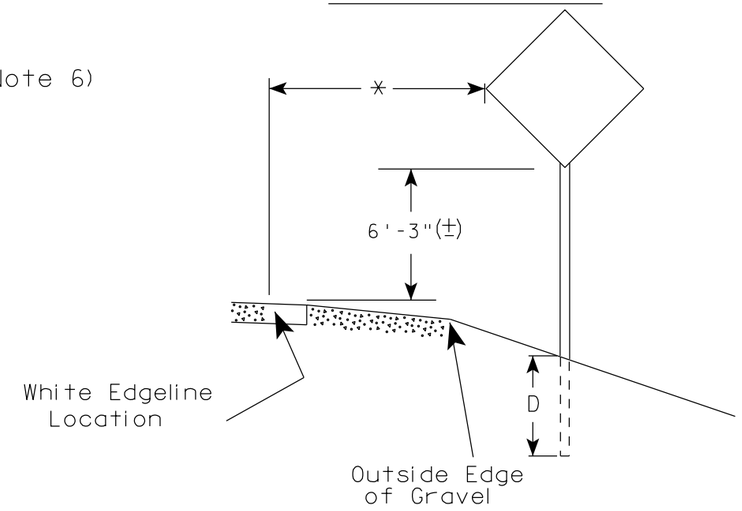
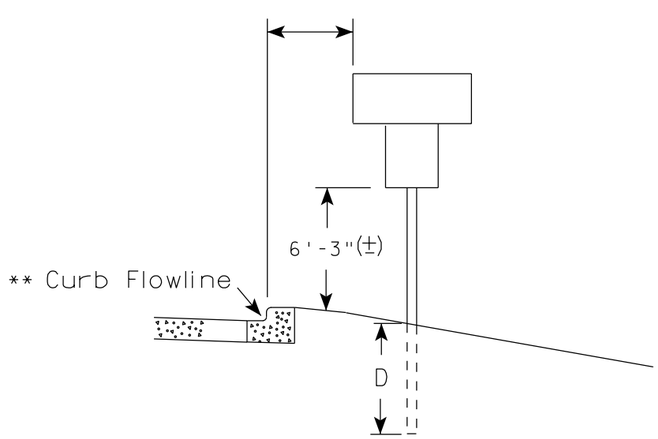
S.D.D. 15 C 8-18a

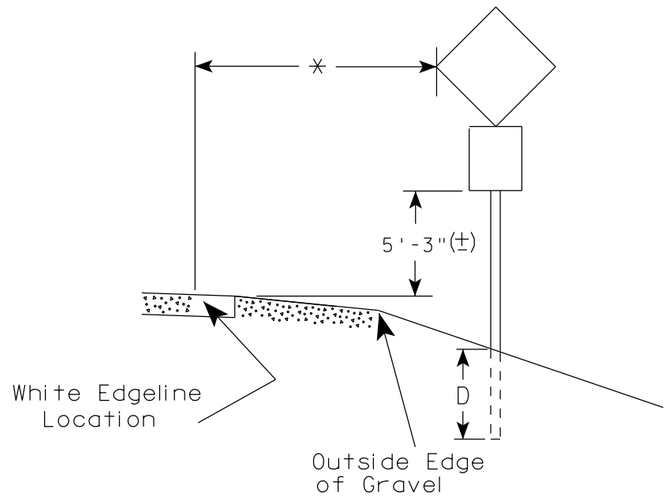
URBAN AREA



RURAL AREA (See Note 2)







GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

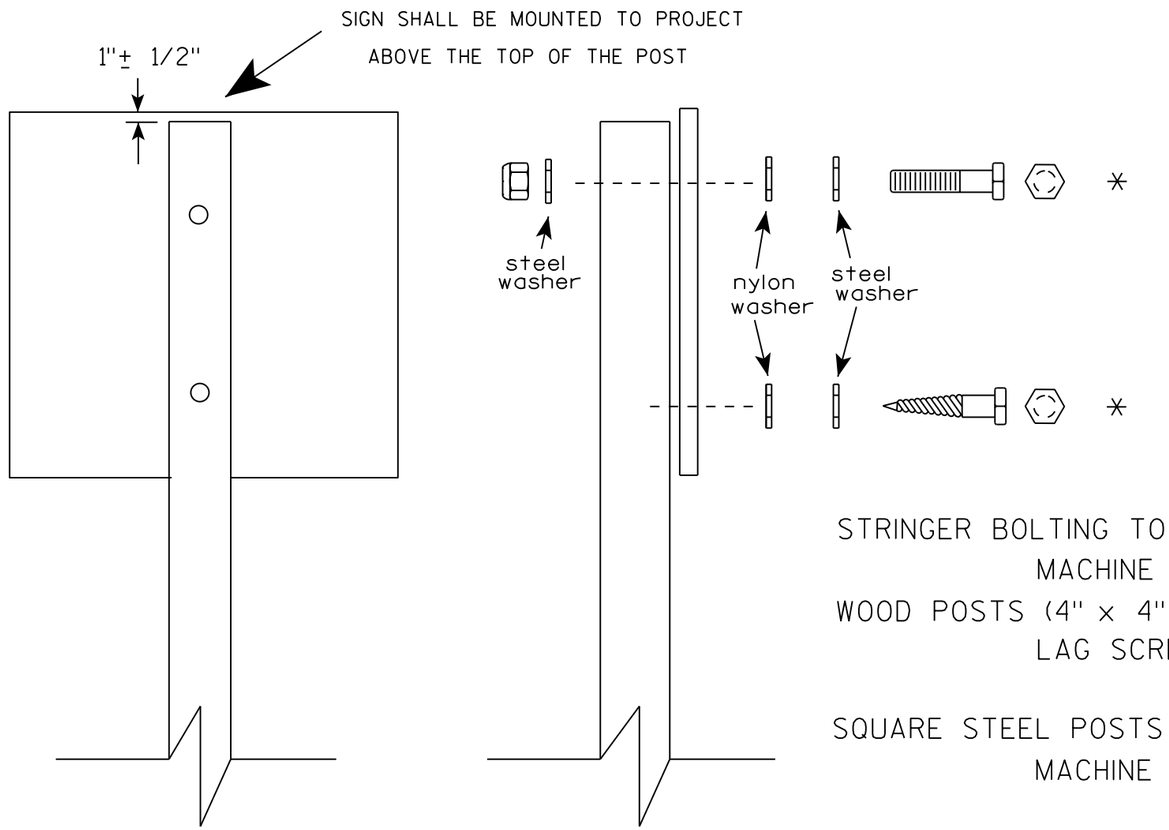
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-3.20

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)

3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)

3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

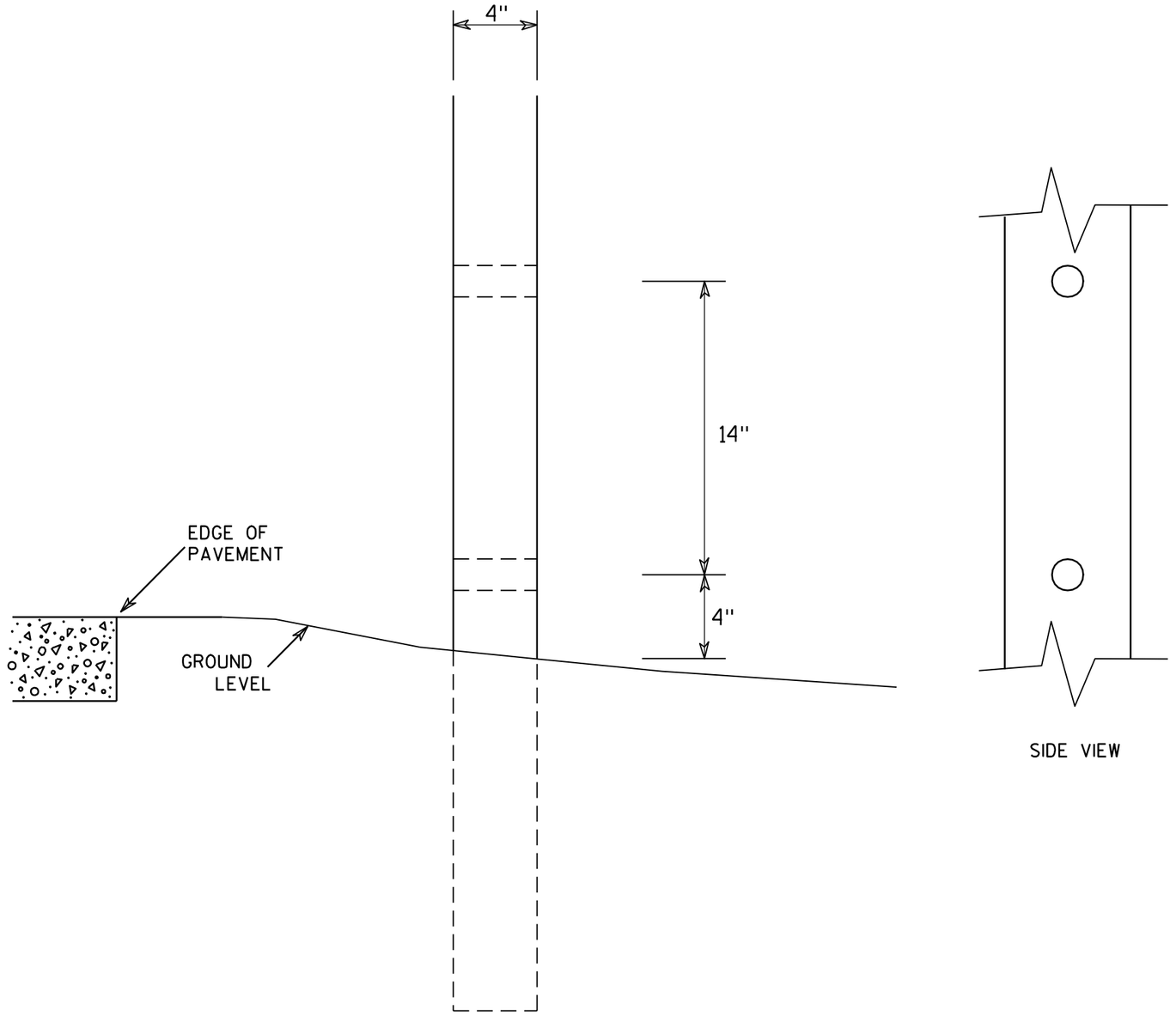
ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/11/16 PLATE NO. A4-8.8

PROJECT NO: SHEET NO: E



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

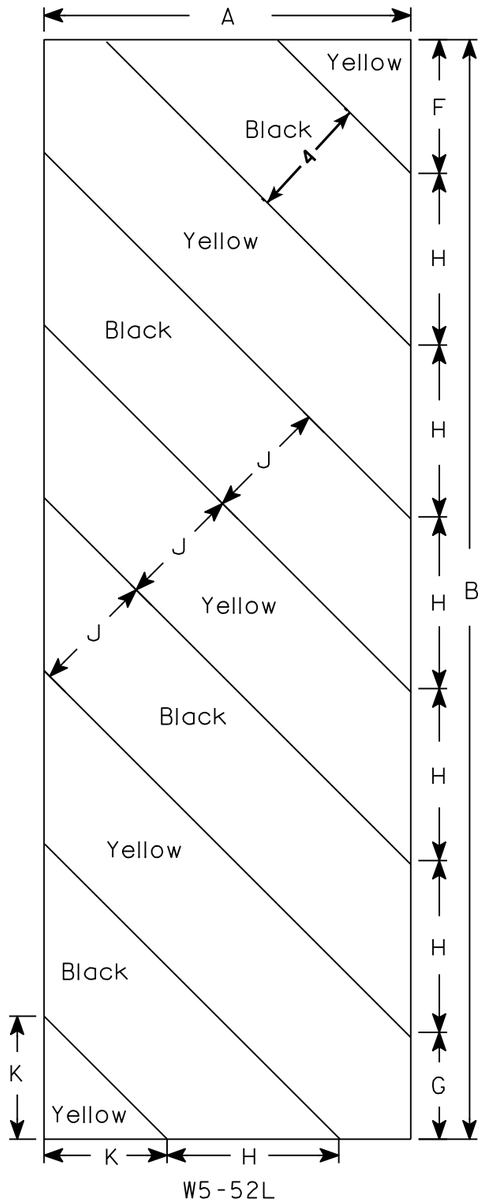
4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

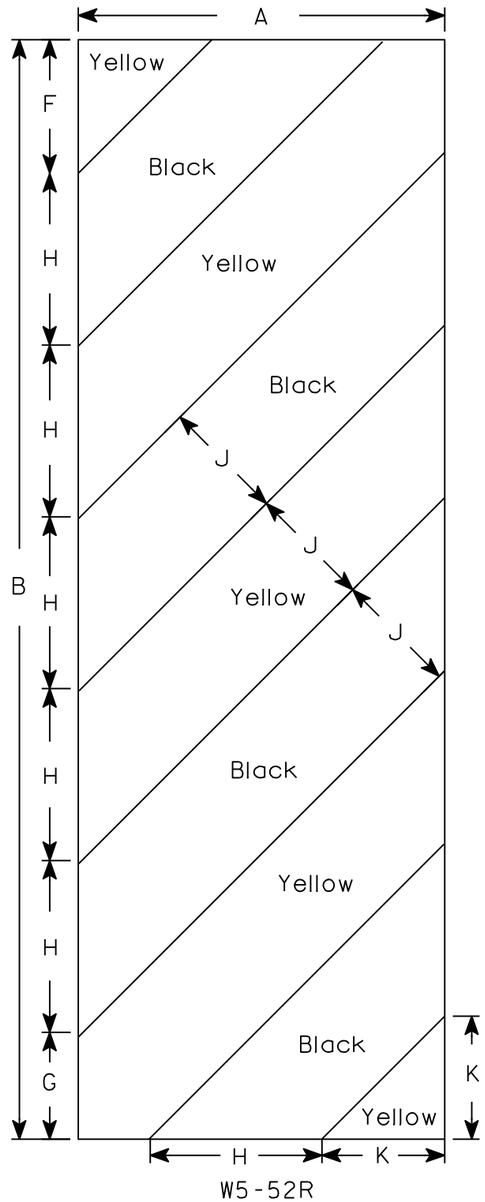
APPROVED Chester J Spang for State Traffic Engineer

DATE 3/27/97 PLATE NO. A4-11.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 21	E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN

W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

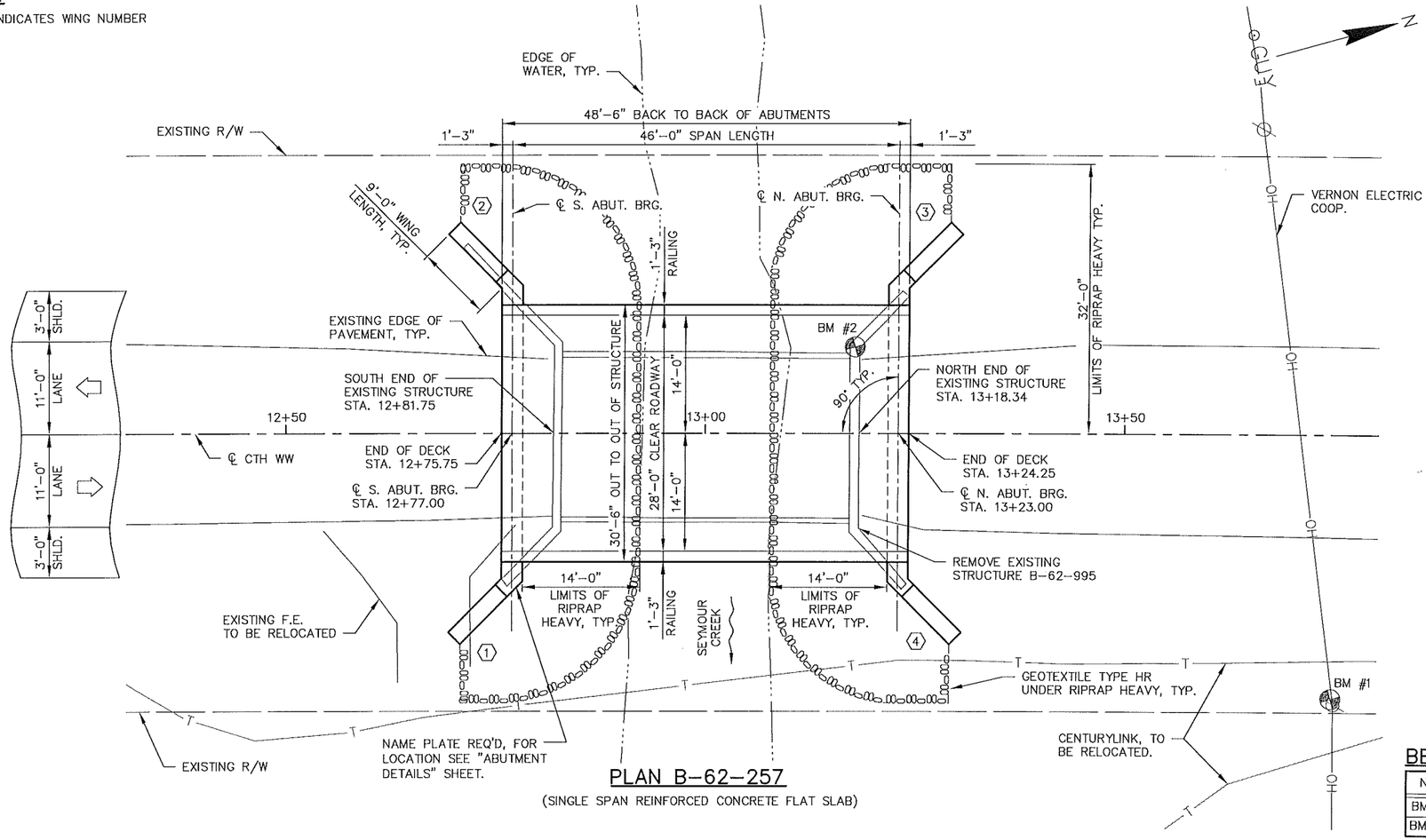
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 22 | E

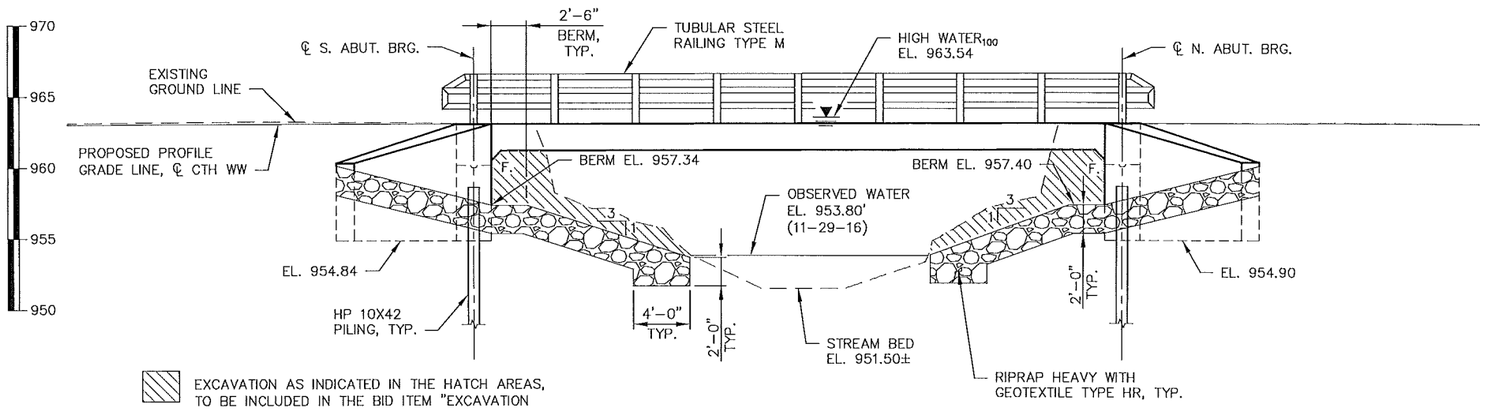
NOTE

⬡ – INDICATES WING NUMBER



PLAN B–62–257

(SINGLE SPAN REINFORCED CONCRETE FLAT SLAB)



ELEVATION

(NORMAL TO SEYMOUR CREEK, LOOKING WEST)

DESIGN DATA

STATE PROJECT NUMBER

5539–00–70

LIVE LOAD:

DESIGN LOADING	HL–93
INVENTORY RATING FACTOR	RF=1.28
OPERATING RATING FACTOR	RF=1.66
WISCONSIN STANDARD PERMIT VEHICLE RATING (WIS.–SPV):	250 KIPS

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY, SLAB	$f'c$ = 4,000 P.S.I.
ALL OTHER	$f'c$ = 3,500 P.S.I.
HIGH–STRENGTH BAR STEEL REINFORCEMENT	f_y = 60,000 P.S.I.

FOUNDATION DATA:

ABUTMENTS TO BE SUPPORTED ON 10–INCH X 42 LB STEEL PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 160 TONS* PER PILE AT THE SOUTH ABUTMENT AND 160 TONS* PER PILE AT THE NORTH ABUTMENT AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 35 FT PILE LENGTHS AT SOUTH ABUTMENT AND 30 FT PILE LENGTHS AT NORTH ABUTMENT.

*THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES DYNAMIC FORMULA TO DETERMINE DRIVEN PILE CAPACITY.

HYDRAULIC DATA:

Q_{100}	2500 C.F.S.
Q_{100} (THRU BRIDGE)	1905 C.F.S.
Q_{100} (ROAD)	595 C.F.S.
DRAINAGE AREA	14.4 SQ. MI.
BRIDGE WATER AREA	266 SQ. FT.
BRIDGE VELOCITY	7.20 F.P.S.
HIGH WATER$_{100}$ EL.	963.54
OVERTOPPING Q	1625 C.F.S.
OVERTOPPING EL.	962.77 FT
OVERTOPPING RDWY	40 YR
SCOUR CRITICAL CODE	5
Q_2	770 C.F.S
Q_2 ELEVATION	958.96 FT
Q_2 VELOCITY	4.4 F.P.S

TRAFFIC DATA:

A.A.D.T. (2019)	200
A.A.D.T. (2039)	230
DESIGN SPEED	40 M.P.H.

WISCONSIN
ANDREW C. KNUTSON
E-34662
SPRING GREEN, WI
5-30-18
PROFESSIONAL ENGINEER

BENCH MARKS

NO.	STATION	DESCRIPTION	ELEVATION
BM #1	13+75.17	RAILROAD SPIKE, 31.16' RT.	960.88
BM #2	13+11.25	CHISELED CROSS ON NW WING WALL, 10.39' LT.	963.59

HORIZONTAL DATUM AND ADJUSTMENT: NAD 83 (2007)
VERTICAL DATUM AND ADJUSTMENT: NAVD 88 (2007)
COORDINATE REFERENCE SYSTEM: VERNON COUNTY

LIST OF DRAWINGS

NO.	DATE	REVISION	BY

WESTBROOK Associated Engineers, Inc.
619 EAST HOXIE STREET
P.O. BOX 429
SPRING GREEN, WI 53588
PHONE (608) 588–7866
FAX (608) 588–7954

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED William C. Dreher SDR 08/15/18
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B–62–257

CTH WW OVER SEYMOUR CREEK

COUNTY	TOWN/CITY/VILLAGE
VERNON	HILLSBORO

DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPEC.

DESIGNED BY	DESIGN CK'D.	DRAWN BY	PLANS CK'D.
JDO	CDS	JDO	CDS

GENERAL PLAN

SHEET 1 OF 8

23

BRIDGE OFFICE CONTACT
WILLIAM DREHER, P.E.
(608) 266–8489

CONSULTANT CONTACT
ANDY KNUTSON, P.E., S.E.
(608) 588–7866

I.D. 5539–00–70

PLOT DATE: May 29, 2018

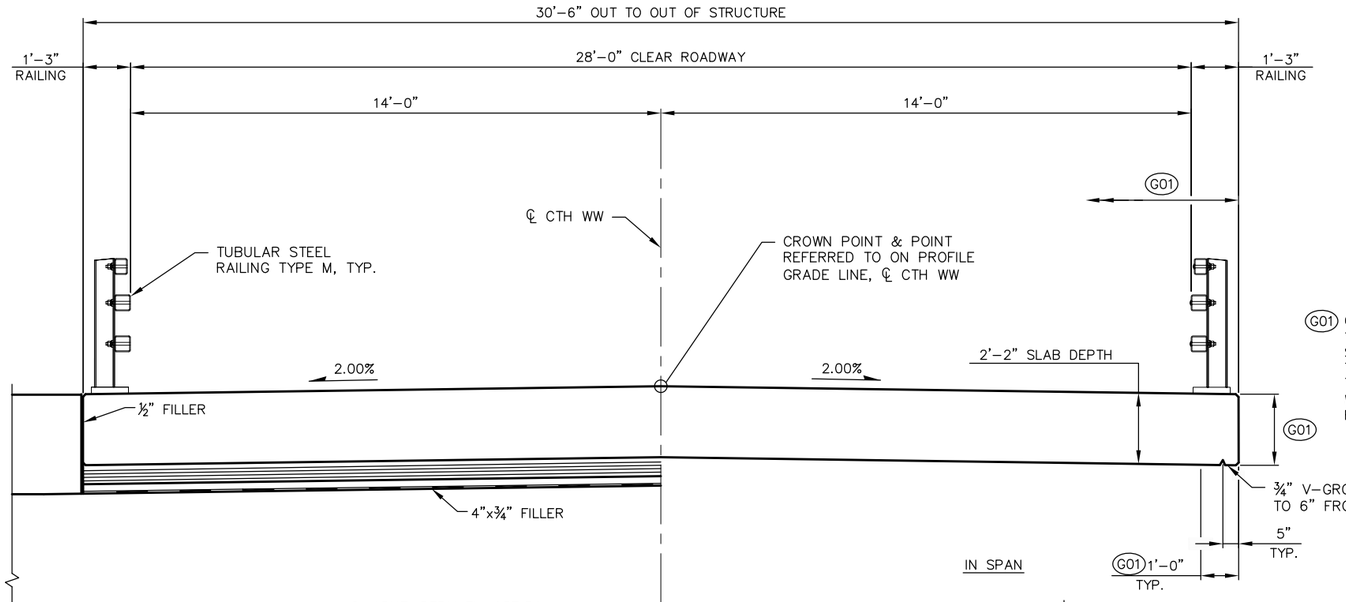
FILE: 01_B620257_gen1.dwg
PLOT SCALE:

8

8

STATE PROJECT NUMBER
5539–00–70

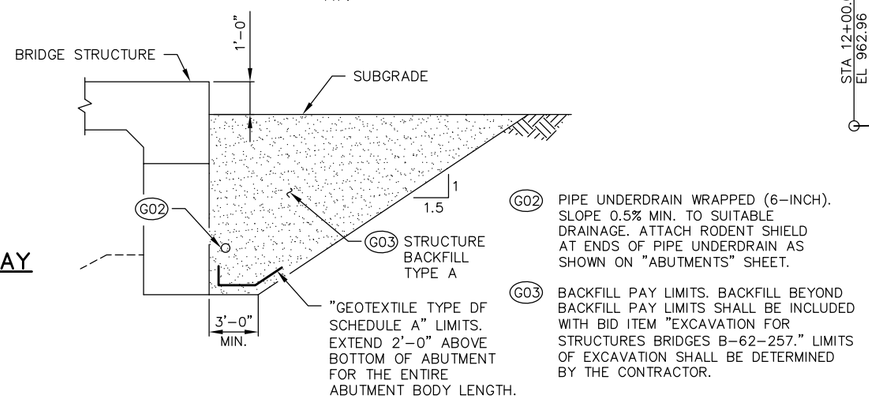
B.F. – BACK FACE
F.F. – FRONT FACE



CROSS SECTION THRU ROADWAY

(LOOKING NORTH)

G01 COAT WITH "PROTECTIVE SURFACE TREATMENT" AS PER THE STANDARD SPECIFICATIONS. PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE TOP AND EXTERIOR EXPOSED FACE OF WINGS, AND THE END 1'–0" OF THE FRONT FACE OF ABUTMENT.



G02 PIPE UNDERDRAIN WRAPPED (6–INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS SHOWN ON "ABUTMENTS" SHEET.

G03 BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCLUDED WITH BID ITEM "EXCAVATION FOR STRUCTURES BRIDGES B–62–257." LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

STRUCTURAL BACKFILL DETAIL

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF A FOUR DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.

THE SLOPE IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH RIPRAP HEAVY AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON THE "GENERAL PLAN" SHEET AND THE "ABUTMENTS" SHEET.

THE FINISHED GRADED SECTION SHALL BE THE UPPER LIMITS OF EXCAVATION FOR STRUCTURES.

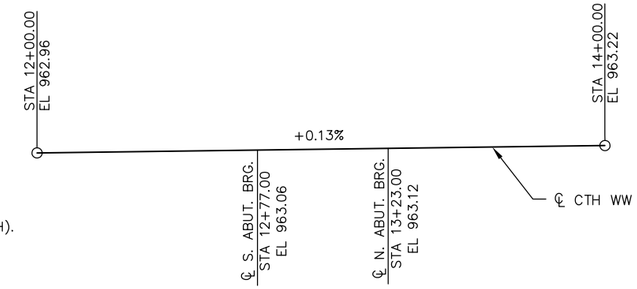
AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE, UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

AT THE BACKFACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH BACKFILL STRUCTURE TYPE A.

THE EXISTING STRUCTURE (B–62–995) IS A SINGLE SPAN STEEL GIRDER CONCRETE DECK STRUCTURE WITH AN OVERALL LENGTH OF 36–FT AND A CLEAR ROADWAY WIDTH OF 19–FT TO BE REMOVED IN ACCORDANCE WITH THE SPECIFICATIONS.

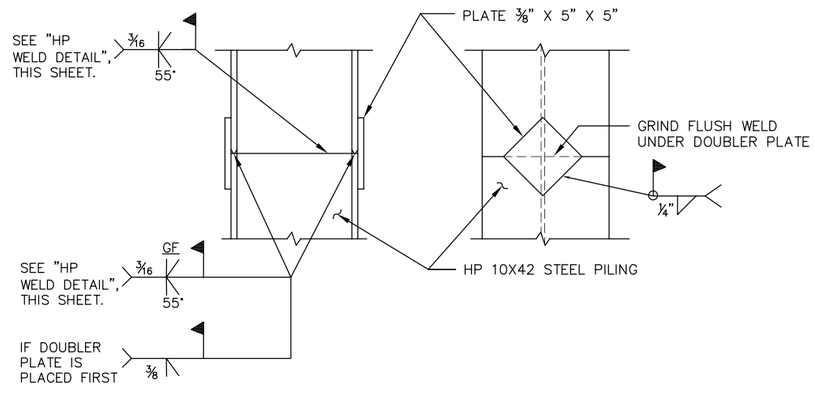
DO NOT PLACE FILL ABOVE 3'–0" FROM BOTTOM OF ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.



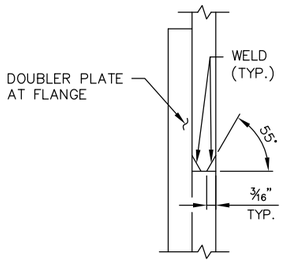
PROFILE GRADE LINE, ℄ CTH WW

TOTAL ESTIMATED QUANTITIES

ITEM NO.	BID ITEMS	UNIT	S. ABUT.	N. ABUT.	SUPER.	TOTALS
203.0600.S	REMOVING OLD STRUCTURE OVER WATERWAY WITH MINIMAL DEBRIS STA. 13+00	LS	---	---	---	1
206.1000	EXCAVATION FOR STRUCTURES BRIDGES B–62–257	LS	---	---	---	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	170	170	---	300
502.0100	CONCRETE MASONRY BRIDGES	CY	28	28	124	180
502.3200	PROTECTIVE SURFACE TREATMENT	SY	12	12	196	220
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	2200	2200	---	4400
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	1420	1420	23080	25920
513.4061	RAILING TUBULAR TYPE M B–62–257	LF	---	---	102	102
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	7	7	---	14
550.1100	PILING STEEL HP 10–INCH X 42 LB	LF	245	210	---	455
606.0300	RIPRAP HEAVY	CY	78	81	---	159
612.0406	PIPE UNDERDRAIN WRAPPED 6–INCH	LF	75	75	---	150
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	46	46	---	92
645.0120	GEOTEXTILE TYPE HR	SY	140	144	---	284
(NON–BID ITEM)	FILLER	SIZE	---	---	---	½" & ¾"



PILE SPLICE DETAILS



HP WELD DETAIL

(FLANGE SHOWN, WEB SIMILAR)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B–62–257

DRAWN BY JDO | PLANS CK'D CDS

CROSS SECTION, GENERAL NOTES & QUANTITIES

SHEET 2 OF 8

24

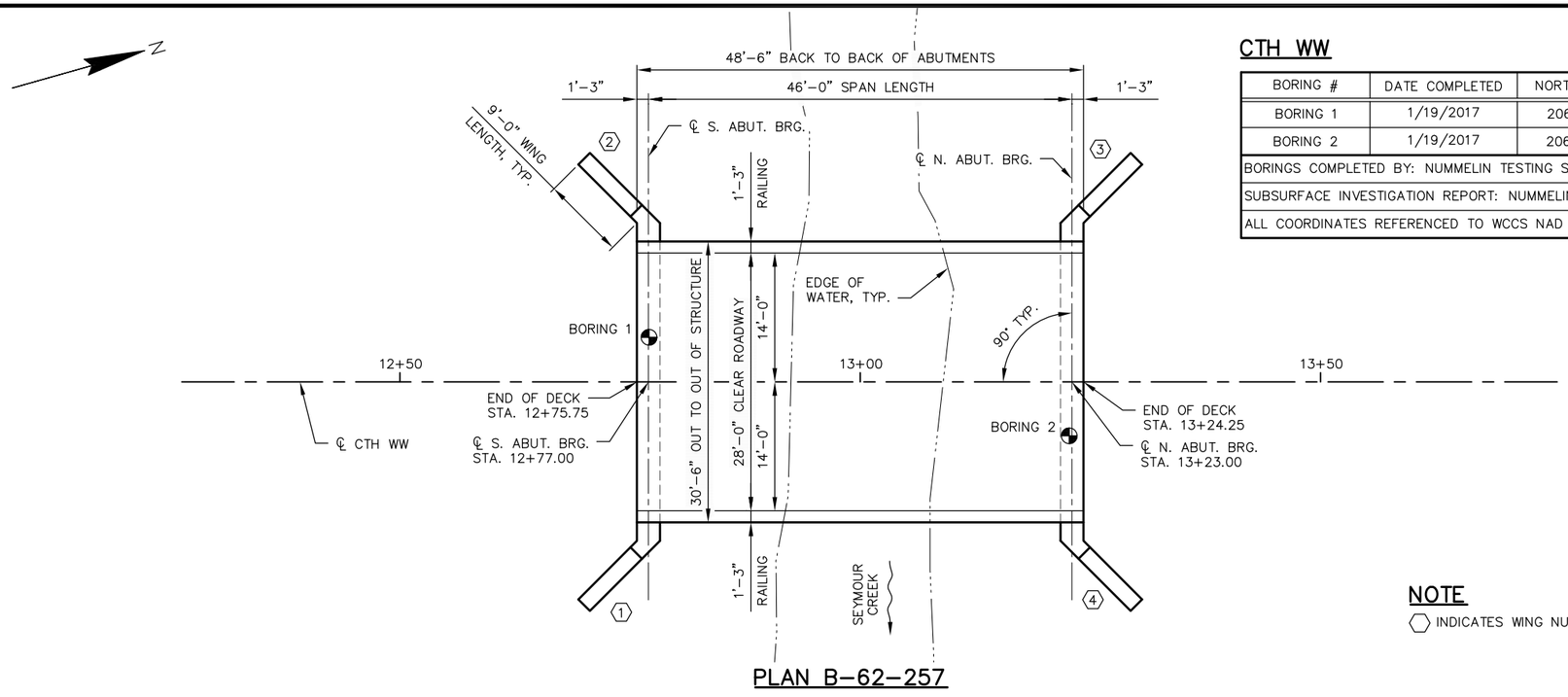
I.D. 5539–00–70

PLOT DATE: May 29, 2018

FILE: 02_B620257_qty.dwg
PLOT SCALE:

8

8



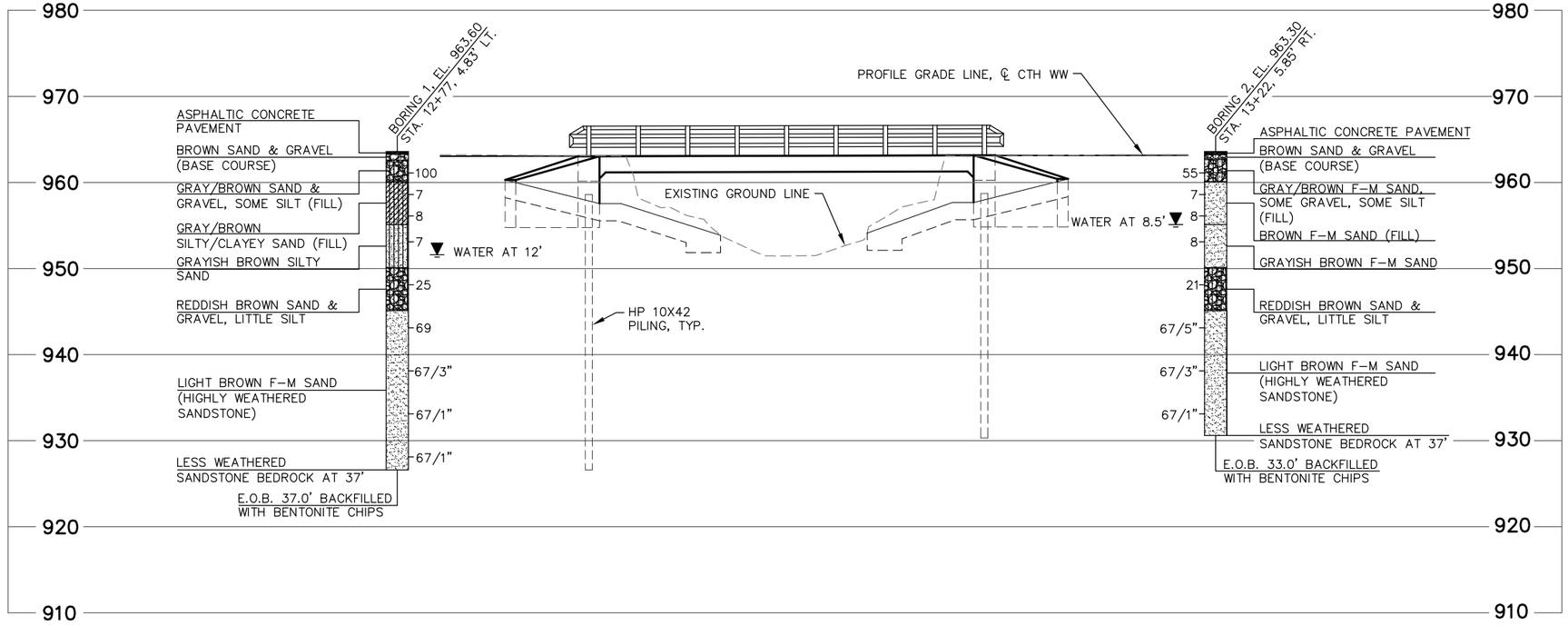
PLAN B-62-257

CTH WW

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
BORING 1	1/19/2017	206531.52	848638.27
BORING 2	1/19/2017	206572.47	848661.09
BORINGS COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
SUBSURFACE INVESTIGATION REPORT: NUMMELIN TESTING SERVICES, INC.			
ALL COORDINATES REFERENCED TO WCCS NAD 83(07) VERNON COUNTY			

NOTE

⬡ INDICATES WING NUMBER



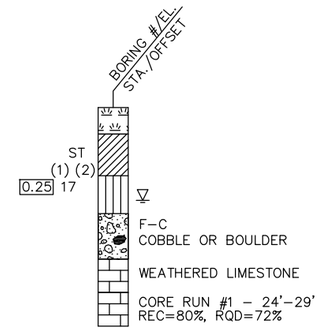
STATE PROJECT NUMBER

5539-00-70

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/ META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

▽ AT TIME OF DRILLING

▼ END OF DRILLING

▽ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-257

DRAWN BY JDO | PLANS CK'D CDS

SUBSURFACE EXPLORATION

SHEET 3 OF 8

25

8

8

I.D. 5539-00-70

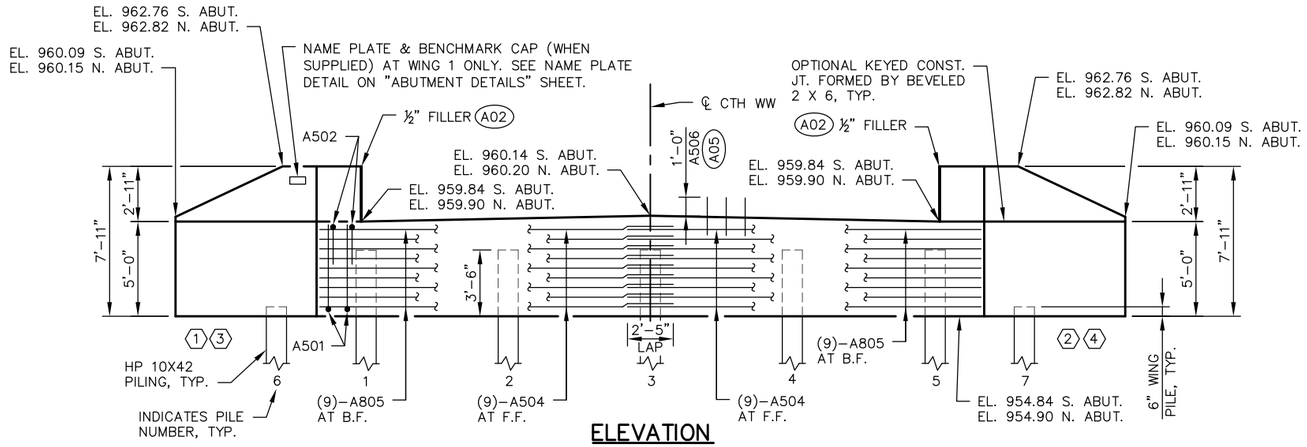
PLOT DATE: May 29, 2018

FILE: 03_B620257_bor.dwg

PLOT SCALE:

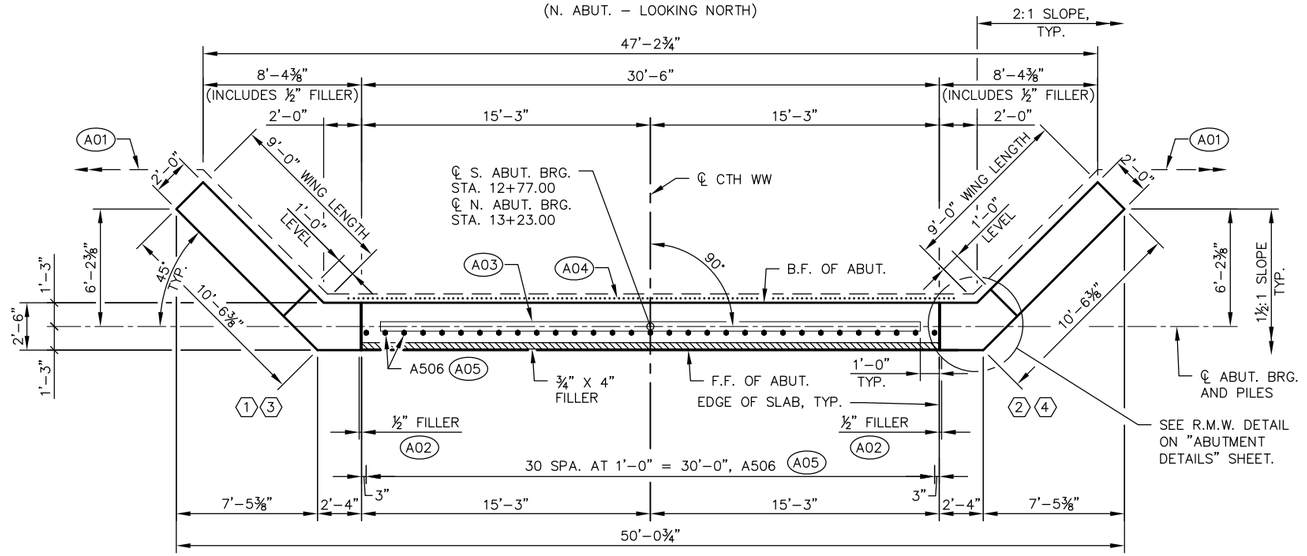
STATE PROJECT NUMBER

5539-00-70

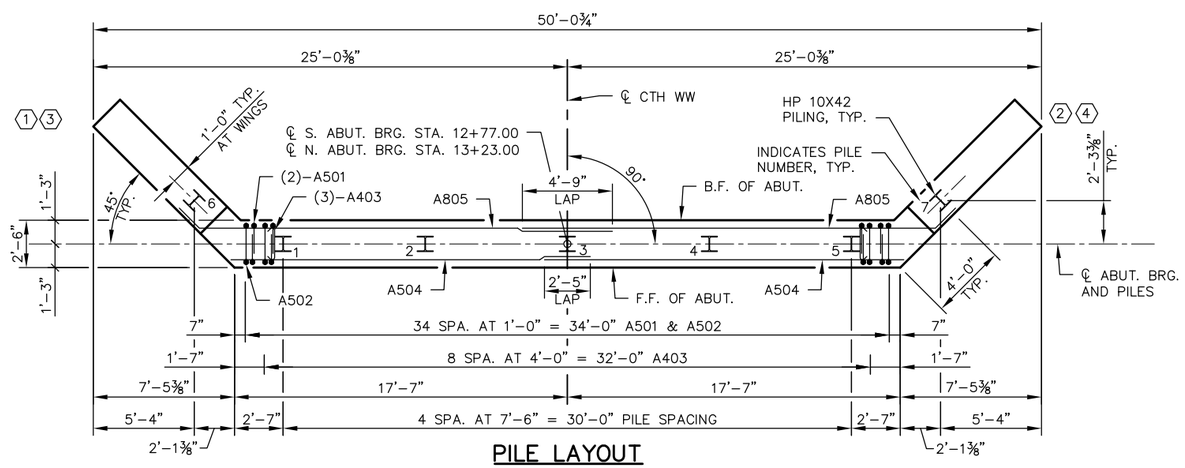


ELEVATION

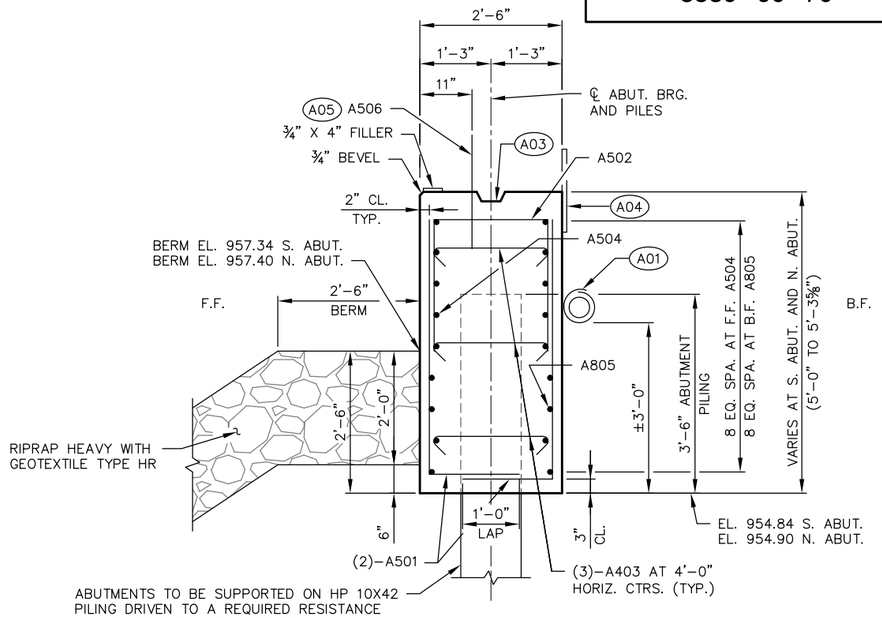
(S. ABUT. – LOOKING SOUTH)
(N. ABUT. – LOOKING NORTH)



PLAN



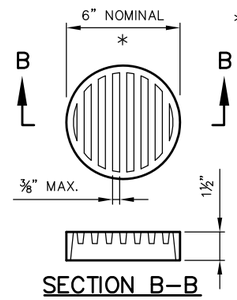
PILE LAYOUT



ABUTMENTS TO BE SUPPORTED ON HP 10X42 PILING DRIVEN TO A REQUIRED RESISTANCE OF 160 TONS PER PILE. ESTIMATE 35 FT. PILE LENGTHS AT THE S. ABUT. AND 30 FT. PILE LENGTHS AT THE N. ABUT.

TYPICAL SECTION THRU ABUTMENT

F.F. – FRONT FACE
B.F. – BACK FACE



SECTION B–B

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SHIELD SO SLOTS ARE VERTICAL.

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

THE RODENT SHIELD. PIPE COUPLING AND SCREWS SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

RODENT SHIELD DETAIL

NOTES

DO NOT PLACE FILL ABOVE 3'–0" FROM THE BOTTOM OF THE ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

(A01) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE, ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

(A02) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE)

(A03) KEYED CONSTRUCTION JOINT FORMED BY A BEVELED 2" X 6".

(A04) 18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W.), SEAL ALL VERTICAL AND HORIZONTAL JOINTS ON BACK FACE.

(A05) A506 BARS MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE IT HAS TAKEN ITS INITIAL SET. EMBED BAR 1'–0".

⬡ INDICATES WING NUMBER

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-257

DRAWN BY JDO | PLANS CK'D CDS

ABUTMENTS

SHEET 4 OF 8

26

I.D. 5539-00-70

PLOT DATE: May 29, 2018

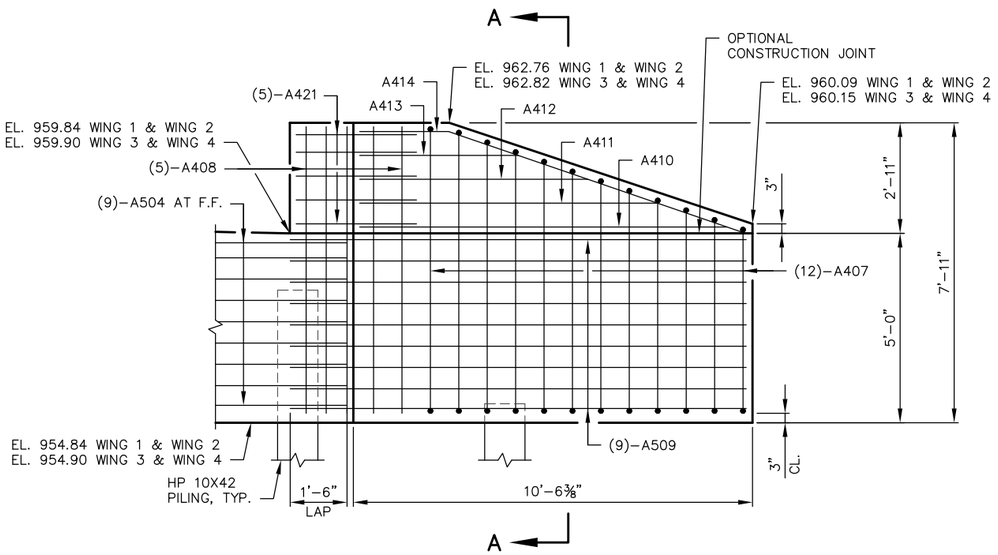
FILE: 04-05_B620257_abut.dwg
PLOT SCALE:

8

8

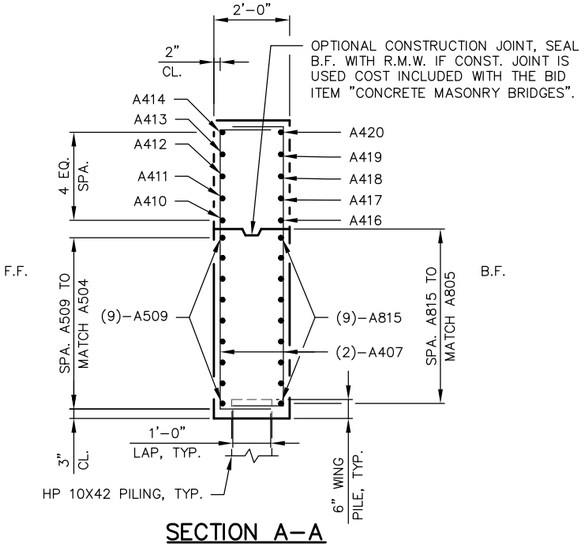
STATE PROJECT NUMBER

5539-00-70



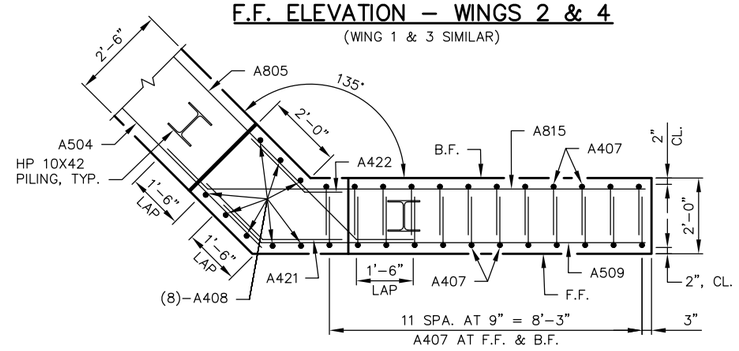
F.F. ELEVATION – WINGS 2 & 4

(WING 1 & 3 SIMILAR)



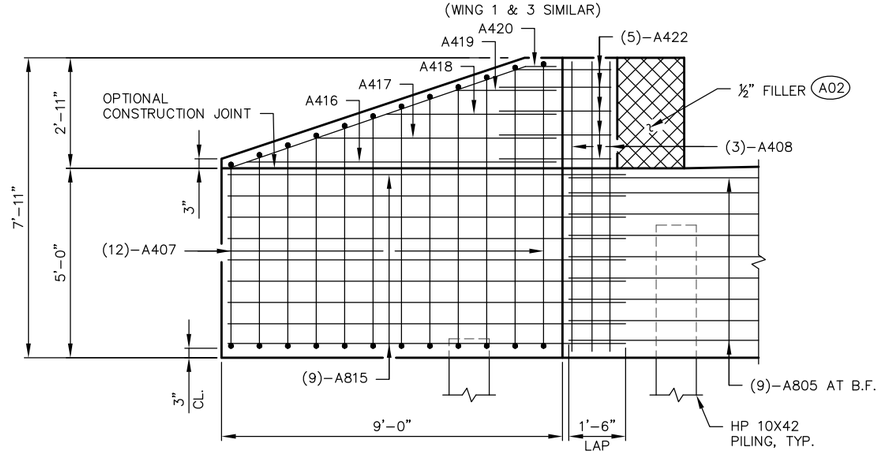
SECTION A-A

F.F. – FRONT FACE
B.F. – BACK FACE



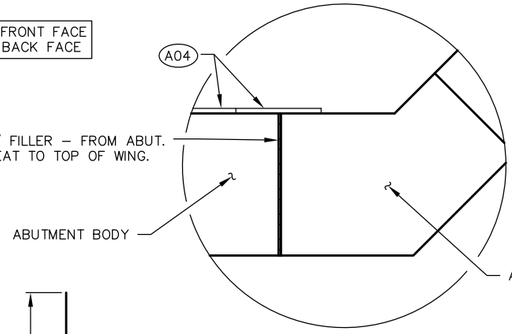
PLAN – WINGS 2 & 4

(WING 1 & 3 SIMILAR)

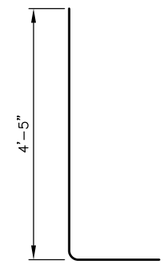


B.F. ELEVATION – WINGS 2 & 4

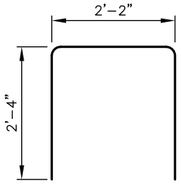
(WING 1 & 3 SIMILAR)



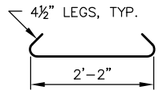
R.M.W. DETAIL



A501



A502



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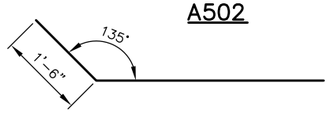
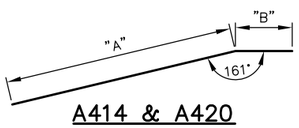


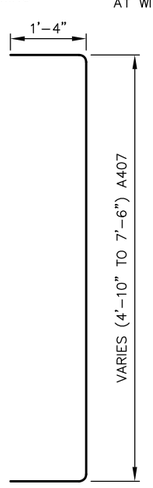
A805, A509 & A815



A414 & A420

BAR BEND DIMENSIONS

MARK	"A"	"B"
A414	8'-3"	2'-4"
A420	8'-3"	0'-9"



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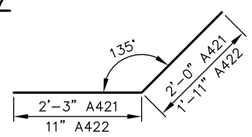


A421 & A422

BILL OF BARS
BOTH ABUTMENTS

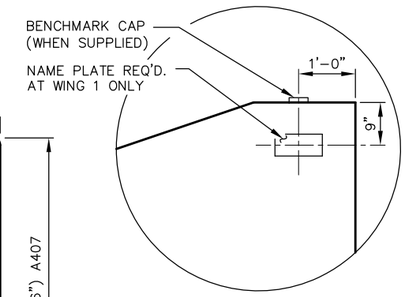
COATED = 2,840 LBS.
UNCOATED = 4,400 LBS.

MARK	NUMBER COATED	NUMBER UNCOATED	LENGTH	BENT	BAR SERIES	LOCATION	
A501		140	5'-11"	X		BODY STIRRUP – F.F. & B.F.	VERT.
A502		70	6'-7"	X		BODY STIRRUP – TOP	VERT.
A403		54	2'-11"	X		BODY – TIES	HORIZ.
A504		36	18'-10"			BODY – F.F.	HORIZ.
A805		36	23'-4"	X		BODY – B.F.	HORIZ.
A506	62		2'-0"			BODY – DOWELS	VERT.
A407	96		8'-8"	X	⚠	WINGS 1 THRU 4 – STIRRUP – F.F. & B.F.	VERT.
A408	32		7'-6"			WINGS 1 THRU 4 – F.F. & B.F.	VERT.
A509	36		11'-9"	X		WINGS 1 THRU 4 – F.F.	HORIZ.
A410	4		9'-8"			WINGS 1 THRU 4 – F.F.	HORIZ.
A411	4		7'-10"			WINGS 1 THRU 4 – F.F.	HORIZ.
A412	4		6'-0"			WINGS 1 THRU 4 – F.F.	HORIZ.
A413	4		4'-2"			WINGS 1 THRU 4 – F.F.	HORIZ.
A414	4		10'-7"	X		WINGS 1 THRU 4 – F.F. – TOP	HORIZ.
A815	36		13'-3"	X		WINGS 1 THRU 4 – B.F.	HORIZ.
A416	4		8'-2"			WINGS 1 THRU 4 – B.F.	HORIZ.
A417	4		6'-3"			WINGS 1 THRU 4 – B.F.	HORIZ.
A418	4		4'-5"			WINGS 1 THRU 4 – B.F.	HORIZ.
A419	4		2'-7"			WINGS 1 THRU 4 – B.F.	HORIZ.
A420	4		9'-0"	X		WINGS 1 THRU 4 – B.F. – TOP	HORIZ.
A421	20		4'-2"	X		WINGS 1 THRU 4 – F.F. CORNER	HORIZ.
A422	20		2'-9"	X		WINGS 1 THRU 4 – B.F. CORNER	HORIZ.

THE FIRST DIGIT OF A BAR MARK SIGNIFIES THE BAR SIZE.

ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.

⚠ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE "BAR SERIES TABLE" FOR ACTUAL LENGTHS.



NAME PLATE DETAIL

NOTES

(A02) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE.)

(A04) 18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W.). SEAL ALL VERTICAL AND HORIZONTAL JOINTS ON BACK FACE.

BAR SERIES TABLE

MARK	NO. REQ'D	LENGTH
A407	8 SERIES OF 12	7'-4" TO 10'-0"

BUNDLE AND TAG EACH SERIES SEPARATELY.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-257

DRAWN BY JDO | PLANS CK'D CDS

ABUTMENT DETAILS

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FILE: 04-05_B620257_abut.dwg
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I.D. 5539-00-70

PLOT DATE: May 29, 2018

STATE PROJECT NUMBER
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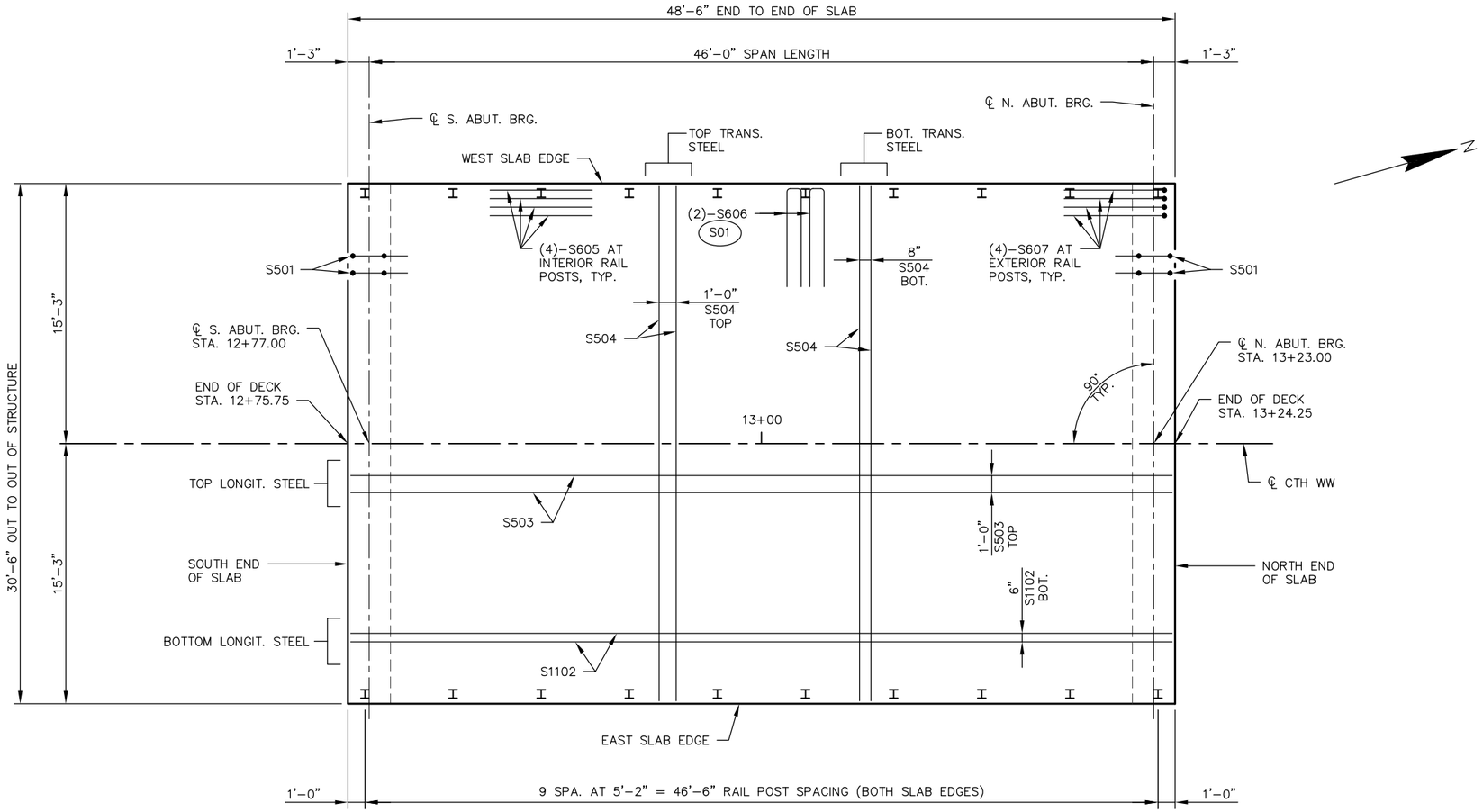
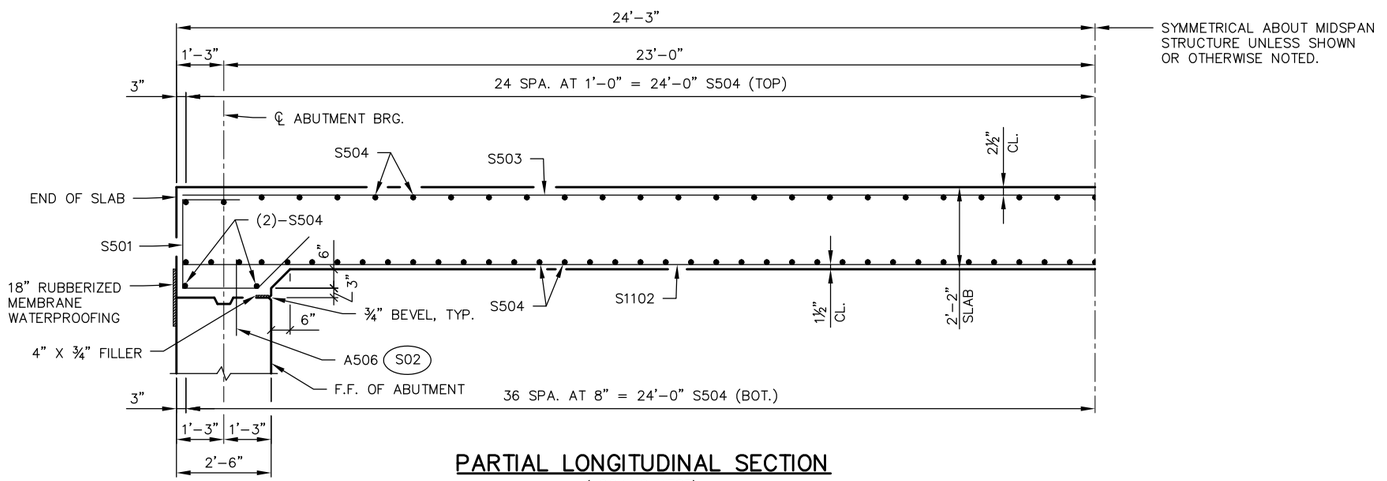


PLAN



PARTIAL LONGITUDINAL SECTION

(LOOKING WEST)

NOTES

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'–0" CENTERS EACH WAY.

BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'–0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

RAILING TO BE PLACED ON THE SLAB AFTER FALSEWORK HAS BEEN RELEASED.

(S01) ADJUST ORIENTATION OF S606 BAR AT END POST TO ENSURE CLEAR COVER AT END OF DECK.

(S02) SEE "ABUTMENTS" SHEET FOR PLACEMENT OF A506 BARS.

TOP OF DECK ELEVATIONS

SPAN PT	W. SLAB EDGE	℄ CTH WW	E. SLAB EDGE
℄ S. ABUT.	962.76	963.06	962.76
0.1	962.77	963.07	962.77
0.2	962.77	963.07	962.77
0.3	962.78	963.08	962.78
0.4	962.78	963.08	962.78
0.5	962.79	963.09	962.79
0.6	962.79	963.09	962.79
0.7	962.80	963.10	962.80
0.8	962.81	963.11	962.81
0.9	962.81	963.11	962.81
℄ N. ABUT.	962.82	963.12	962.82

F.F. – FRONT FACE
B.F. – BACK FACE

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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B–62–257

DRAWN BY JDO | PLANS CK'D CDS

SUPERSTRUCTURE

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I.D. 5593–00–70

PLOT DATE: May 29, 2018

FILE: 06–07_B620257_super.dwg
PLOT SCALE:

STATE PROJECT NUMBER

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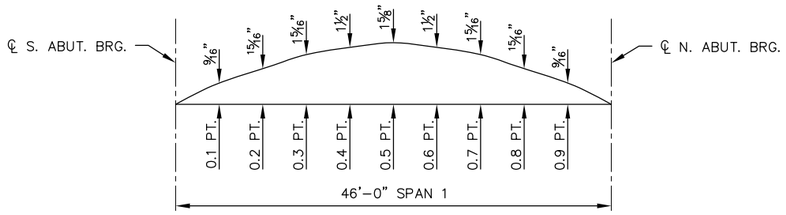


SLAB CAMBER DIAGRAM

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

	TOP OF SLAB ELEVATION AT FINAL GRADE
LESS	SLAB THICKNESS
PLUS	CAMBER
PLUS	FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
EQUALS	TOP OF SLAB FALSEWORK ELEVATION.

SURVEY TOP OF SLAB ELEVATIONS

	℄ S. ABUT. BRG.	5/10 PT.	℄ N. ABUT. BRG.
WEST SLAB EDGE			
℄ CTH WW			
EAST SLAB EDGE			

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF DECK ELEVATIONS AT THE ℄ OF ABUTMENTS AND AT 5/10 POINTS TO VERIFY CAMBER. TAKE ELEVATIONS ALONG EDGE OF SLAB AND REFERENCE LINE. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

BILL OF BARS
SUPERSTRUCTURE

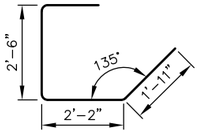
COATED = 23,080 LBS.

MARK	NUMBER COATED	NUMBER UNCOATED	LENGTH	BENT	BAR SERIES	LOCATION	
S501	62		7'–9"	X		SLAB AT ABUTMENT – TIES	LONGIT.
S1102	61		48'–2"			SLAB – BOTTOM	LONGIT.
S503	31		48'–2"			SLAB – TOP	LONGIT.
S504	126		30'–2"			SLAB – TOP & BOTTOM	TRANS.
◑ S605	64		6'–0"			RAILING ANCHORS – INTERIOR POSTS	LONGIT.
◑ S606	40		12'–0"	X		RAILING ANCHORS	TRANS.
◑ S607	16		5'–10"	X		RAILING ANCHORS – END POSTS	LONGIT.

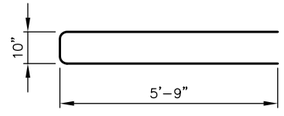
THE FIRST OR FIRST TWO DIGITS OF A FOUR DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.

◑ SEE "SUPERSTRUCTURE" SHEET AND "TUBULAR STEEL RAILING TYPE M" SHEET FOR PLACEMENT.



S501



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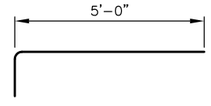

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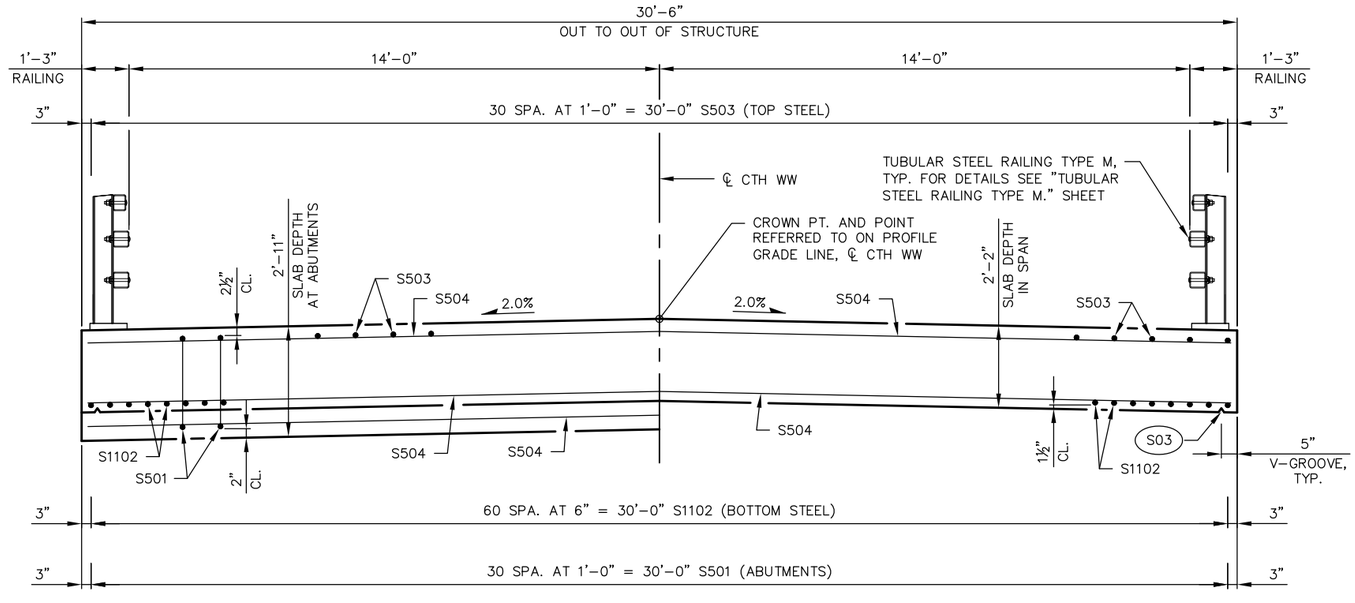

AT ABUTMENTS IN SPAN

CROSS SECTION THRU ROADWAY
(LOOKING NORTH)

NOTES

CAMBER SPAN AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT.

(S03) ¾" V–GROOVE REQUIRED. EXTEND V–GROOVE TO 6" FROM FRONT FACE OF ABUTMENT.

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STATE OF WISCONSIN
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STRUCTURE B–62–257

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SUPERSTRUCTURE DETAILS

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I.D. 5593–00–70

PLOT DATE: May 29, 2018

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PLOT SCALE:

STATE PROJECT NUMBER

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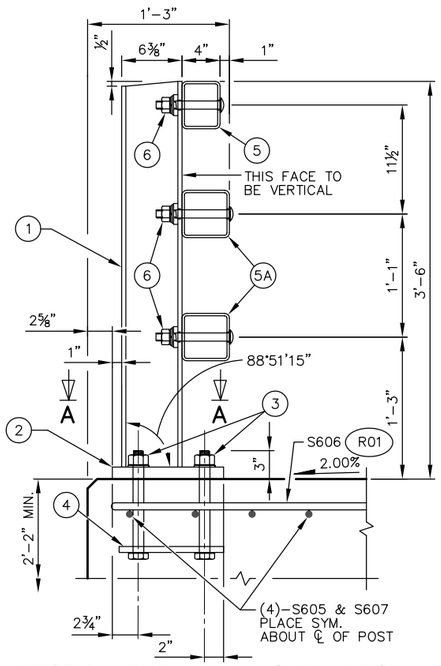
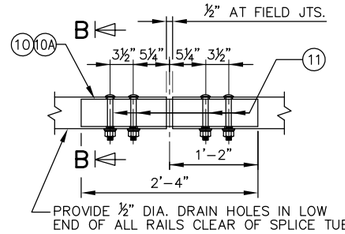
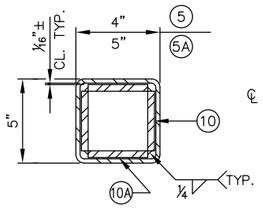


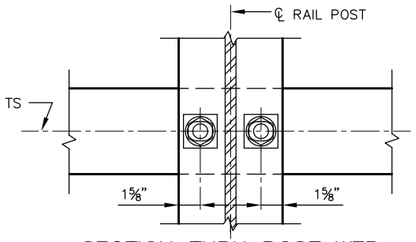
SECTION THRU RAILING ON DECK



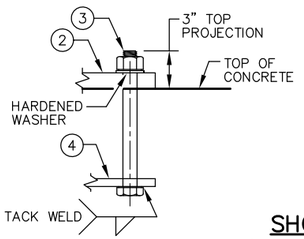
FIELD ERECTION JOINT DETAIL



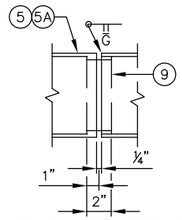
SECTION B–B



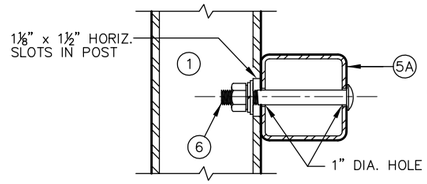
SECTION THRU POST WEB



ANCHOR BOLTS



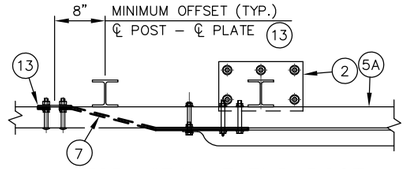
SHOP RAIL SPLICE DETAIL
(LOCATION MUST BE SHOWN ON SHOP DRAWINGS)



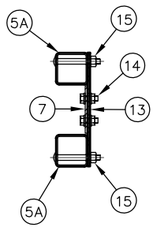
SECTION THRU RAIL

NOTE: CONNECTIONS AT LOWER RAILS SHOWN. CONNECTIONS AT TOP RAIL SIMILAR.

TYPICAL RAIL TO POST CONNECTIONS



TOP VIEW AT END POST
(THRIE BEAM RAIL ATTACHMENT)



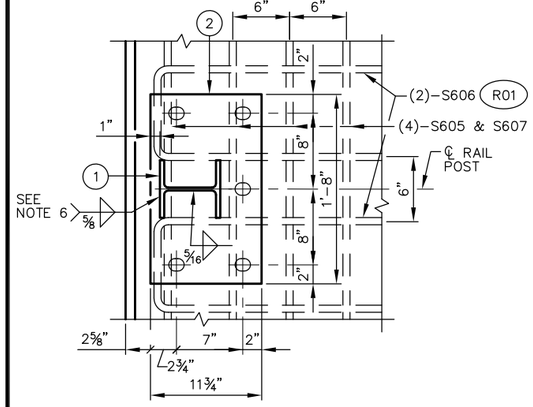
SECTION C–C

LEGEND

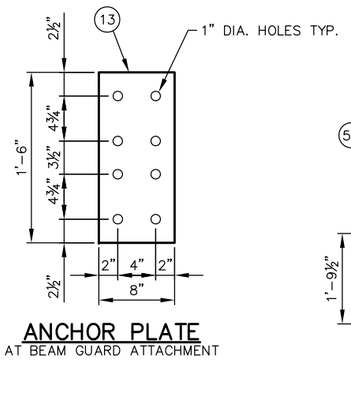
1. W6 x 25 WITH 1⅛" x 1½" HORIZ. SLOTS ON EACH SIDE OF POST FOR BOLT NO. 6. CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF ROADWAY. PLACE POST VERTICAL. PLACE POSTS NORMAL TO GRADE LINE.
2. PLATE 1¼" x 11¾" x 1'-8" WITH 1⁵⁄₁₆" x 1⅝" SLOTTED HOLES FOR ANCHOR BOLTS NO. 3. WELD TO NO. 1 AS SHOWN. SLOTS PARALLEL TO SHORT SIDE OF PLATE.
3. ASTM A449 – 1⅛" DIA. ANCHOR BOLTS WITH NUT AND HARDENED WASHER (ALL GALVANIZED). 5 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 2. CHAMFER TOP OF BOLTS BEFORE THREADING. USE 1'-9" LONG IN ABUTMENT WINGS. AT POSTS ON CONCRETE SLAB SUPERSTRUCTURES WHERE THE SLAB THICKNESS IS > 16" USE 1'-3" LONG. USE 10¾" LONG AT ALL OTHER LOCATIONS. (AN EQUIVALENT THREADED ROD WITH NUTS AND HARDENED WASHERS MAY BE SUBSTITUTED FOR ANCHOR BOLTS IN WINGS IF REQ'D. FOR CONSTRUCTABILITY.)
4. ⅝" x 11" x 1'-8" ANCHOR PLATE (GALVANIZED) WITH 1³⁄₁₆" DIA. HOLES FOR ANCHOR BOLTS NO. 3
5. TS 5 x 4 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
5A. TS 5 x 5 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
6. ⅞" DIA. A325 SLOTTED ROUND HEAD BOLT WITH NUT, 3⁄16" x 1⅝" x 1⅝" WASHER, AND LOCK WASHER (2 REQ'D. AT EACH RAIL TO POST LOCATION.)
7. ½" THK. BACK-UP PLATE WITH 2 – ⅞" x 1½" THREADED SHOP WELDED STUDS (NO. 12). BOLT TO RAIL AS SHOWN IN DETAIL. REQUIRED AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYMMETRICALLY ABOUT TUBES NO. 5A.
8. 1" DIA. HOLES IN PLATE NO. 7 & TUBES NO. 5A FOR ⅞" DIA. A325 BOLTS WITH HEX NUTS AND WASHERS. 6 HOLES IN TUBES AND PLATE NO. 7.
9. SPLICE SLEEVE FABRICATED FROM ¼" PLATE. PROVIDE "SLIDING FIT".
10. ⅜" x 3⅝" x 2'-4" PLATE. 2 PER RAIL. USED IN NO. 5 & 5A.
10A. ⅜" x 2⅝" x 2'-4" PLATE USED IN NO. 5, ⅜" x 3⅝" x 2'-4" PLATE USED IN NO. 5A. 2 PER RAIL.
11. ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER AND LOCK WASHER. USE ⁵⁄₁₆" x 1¼" LONGIT. SLOTTED HOLES AT FIELD JOINTS AND ⁵⁄₁₆" x 2¼" MIN. LONGIT. SLOTTED HOLES AT EXP. JOINTS IN PLATE NO. 10A.
12. ⅞" DIA. x 1½" LONG THREADED SHOP WELDED STUDS (2 REQ'D).
13. ⅜" x 8" x 1'-6" PLATE. BOLT TO RAIL AS SHOWN IN DETAIL. REQUIRED AT THRIE BEAM GUARD RAIL ATTACHMENTS ONLY. PLACE SYMMETRICALLY ABOUT TUBES NO. 5A.
14. ⅞" DIA. x 2" LONG A325 HEX BOLT WITH NUT AND WASHER (5 REQ'D.).
15. 1" DIA. HOLES IN TUBES NO. 5A FOR ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER, AND LOCK NUT (4 REQ'D.). 4 HOLES IN TUBES.

NOTES

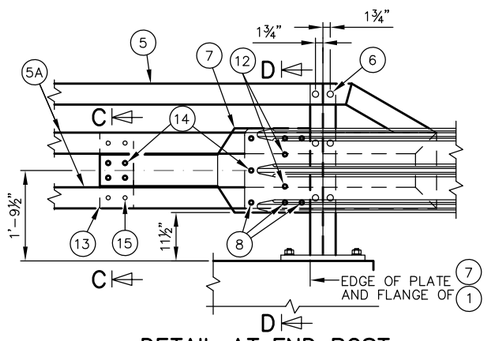
1. BID ITEM SHALL BE "RAILING TUBULAR TYPE M B-62-257" WHICH INCLUDES ALL ITEMS SHOWN.
2. RAIL POST AND BASE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 50. HOLLOW RAILING STRUCTURAL TUBING SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B OR C WITH A CERTIFIED FY = 50 KSI. ANCHOR PLATES, AND SPLICE TUBE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 36.
3. THE NUT SECURING THE POST BASE PLATE TO THE CONCRETE SHALL BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL 1/8 TURN.
4. RAILS SHALL BE CONTINUOUS OVER A MINIMUM OF THREE (3) POSTS WITHOUT SPLICES WHERE POSSIBLE.
5. ENDS OF TUBE SECTIONS SHALL BE SAWED. GRIND SMOOTH EXPOSED EDGES. ALL CUT ENDS SHALL BE TRUE AND SMOOTH.
6. WELD IS THE SAME ON BOTH FLANGES. FLANGE WELD DOES NOT REQUIRE MAGNETIC PARTICLE TESTING.
7. FILL BOLT SLOT OPENINGS IN POST SHIMS AND PLATE NO. 2 AND CAULK AROUND PERIMETER OF PLATE NO. 2 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. STEEL POST SHIMS MAY BE USED UNDER POSTS WHERE REQ'D. FOR ALIGNMENT.
8. POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.
9. ALL MATERIAL SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, ALL STEEL RAILING POSTS & STEEL TUBING SHALL BE GIVEN A NO. 6 BLAST CLEANING BY S.S.P.C. SPECIFICATIONS.
10. SEE BRIDGE MANUAL 30.2 FOR ALLOWED USE.



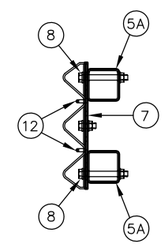
SECTION A–A



ANCHOR PLATE
AT BEAM GUARD ATTACHMENT

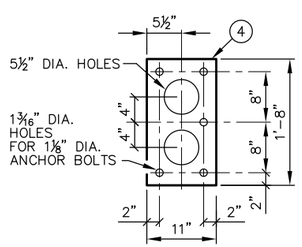


DETAIL AT END POST
(THRIE BEAM RAIL ATTACHMENT)

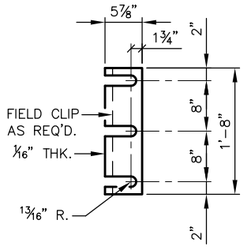


SECTION D–D

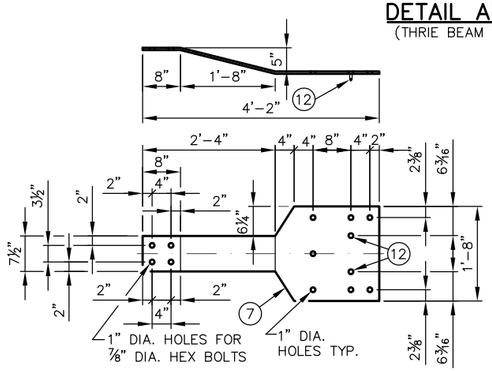
R01 TIE TO TOP MAT OF STEEL.



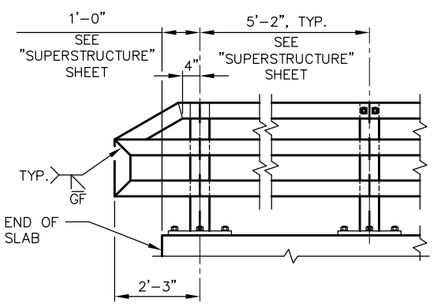
ANCHOR PLATE
AT RAIL TO DECK CONNECTION



POST SHIM DETAIL



BACK-UP PLATE DETAIL
AT BEAM GUARD ATTACHMENTS



PART ELEVATION OF RAILING

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-62-257

DRAWN BY JDO | PLANS CK'D CDS

TUBULAR STEEL RAILING TYPE M

SHEET 8 OF 8

30

I.D. 5593-00-70

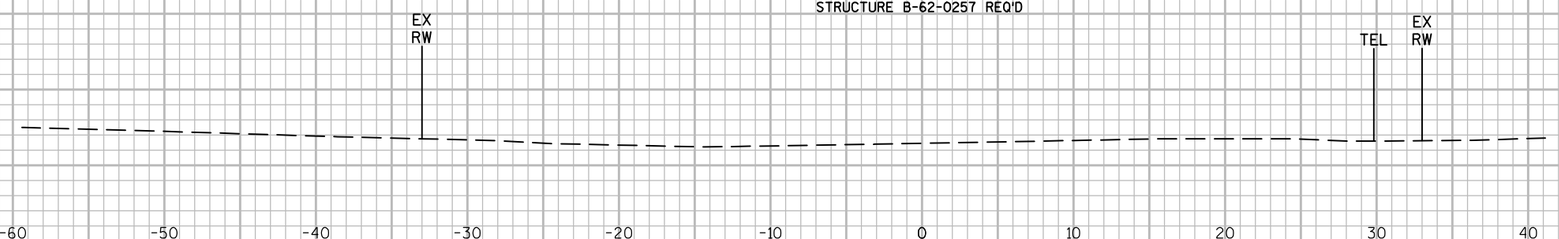
PLOT DATE: May 29, 2018

FILE: 08_B620257_mrail.dwg
PLOT SCALE:

8

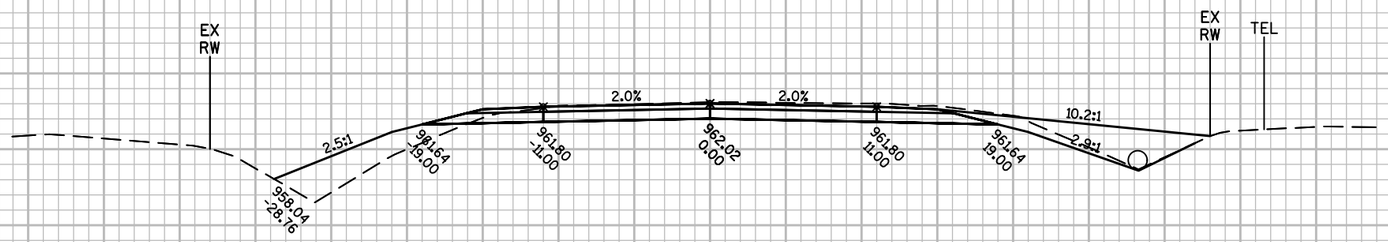
8

STRUCTURE B-62-0257 REQ'D



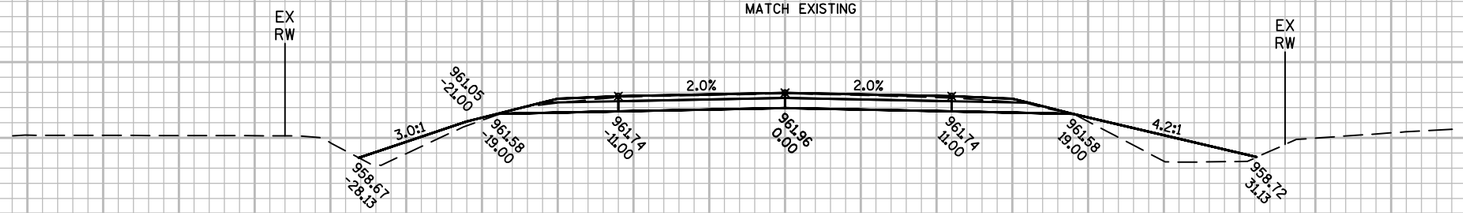
13+00

STA. 12+50, 28.28' RT
CONSTRUCT F.E. RT
1 CPCS, 15-INCH REQ'D
0.064-INCH PIPE THICKNESS

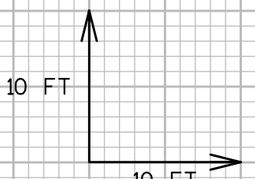


12+50

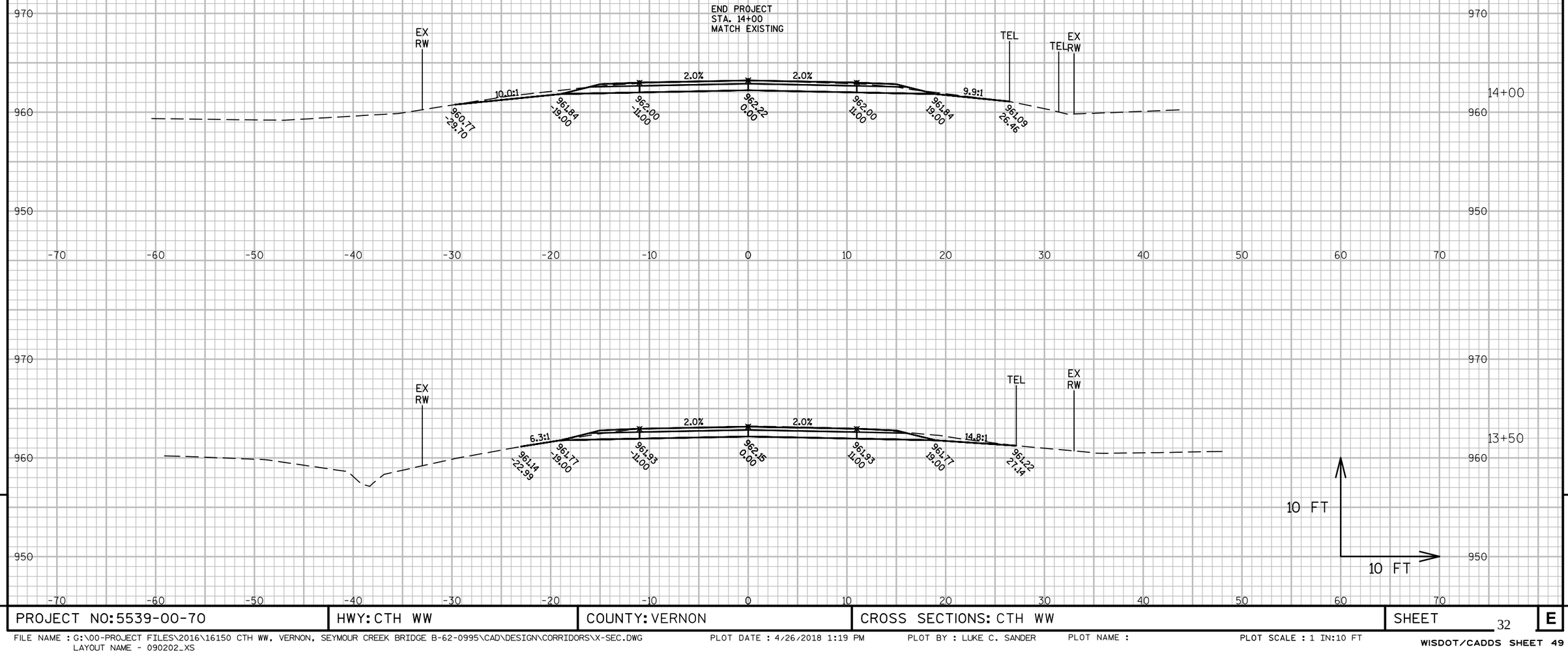
BEGIN PROJECT
STA. 12+00
MATCH EXISTING



12+00



PROJECT NO: 5539-00-70	HWY: CTH WW	COUNTY: VERNON	CROSS SECTIONS: CTH WW	SHEET 31	E



PROJECT NO: 5539-00-70	HWY: CTH WW	COUNTY: VERNON	CROSS SECTIONS: CTH WW	SHEET 32	E

FILE NAME : G:\00-PROJECT FILES\2016\16150 CTH WW, VERNON, SEYMOUR CREEK BRIDGE B-62-0995\CAD\DESIGN\CORRIDORS\X-SEC.DWG
LAYOUT NAME - 090202_XS
PLOT DATE : 4/26/2018 1:19 PM PLOT BY : LUKE C. SANDER PLOT NAME : PLOT SCALE : 1 IN:10 FT

Notes

Wisconsin Department of Transportation

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