

ORDER OF SHEETS

- Section No. 1 Title
- Section No. 2 Typical Sections and Details
- Section No. 3 Estimate of Quantities
- Section No. 3 Miscellaneous Quantities
- Section No. 4 Right of Way Plot
- Section No. 5 Plan and Profile
- Section No. 6 Standard Detail Drawings
- Section No. 7 Sign Plates
- Section No. 8 Structure Plans
- Section No. 9 Computer Earthwork Data
- Section No. 9 Cross Sections

TOTAL SHEETS = 258



25

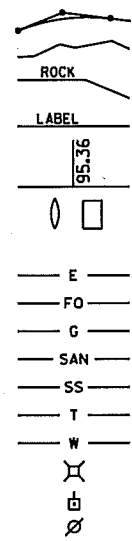
DESIGN DESIGNATION 6054-05-05

A.A.D.T.	2017	=	8600
A.A.D.T.	2027	=	9700
D.H.V.		=	8.3
D.D.		=	
T.		=	
DESIGN SPEED		=	
ESALS		=	

CONVENTIONAL SYMBOLS

- PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS
- MARSH AREA
- WOODED OR SHRUB AREA

- PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such)
- SPECIAL DITCH
- GRADE ELEVATION
- CULVERT (Profile View)
- UTILITIES
- ELECTRIC
- FIBER OPTIC
- GAS
- SANITARY SEWER
- STORM SEWER
- TELEPHONE
- WATER
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE



STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

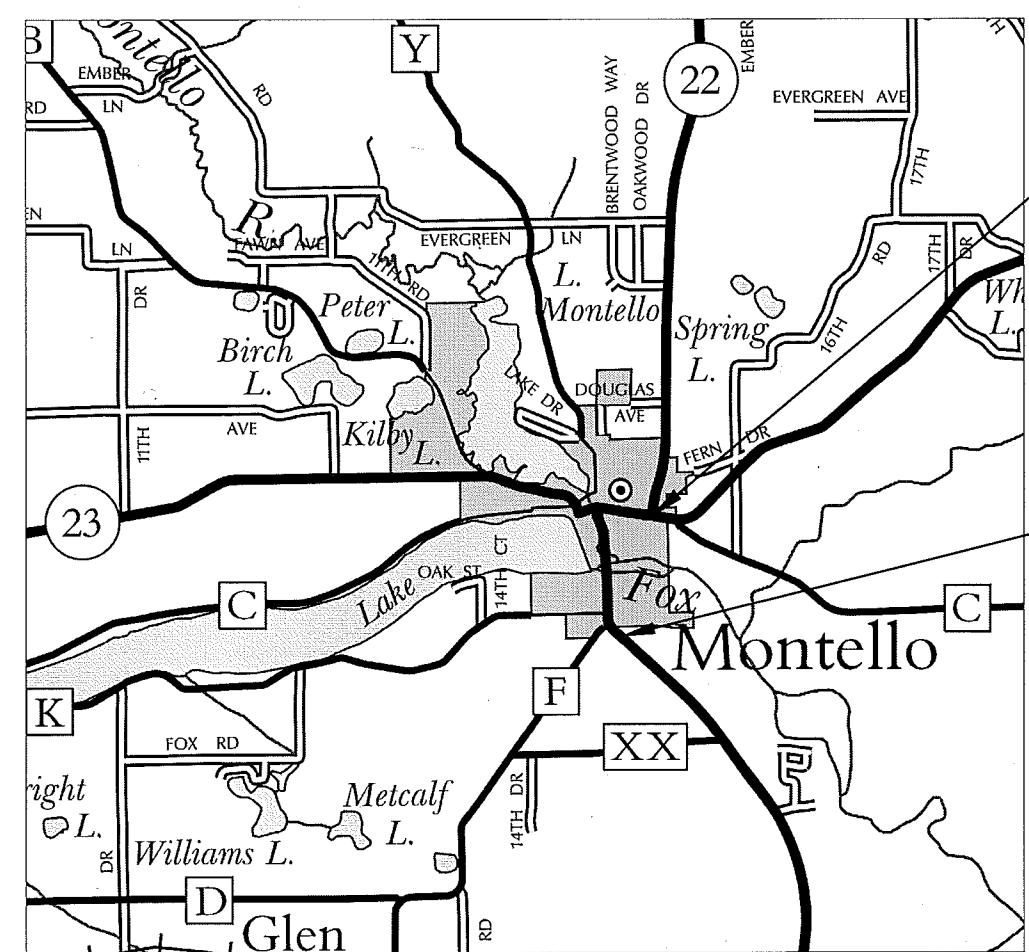
PLAN OF PROPOSED IMPROVEMENT

C MONTELLO, MAIN & MONTELLO STS.

5TH STREET TO N JCT STH 23

STH 22
MARQUETTE

STATE PROJECT NUMBER
6054-05-75



LAYOUT
 SCALE 0 1 MILE

TOTAL NET LENGTH OF CENTERLINE = 1.40

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, MARQUETTE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6054-05-75	WISC 2018250	1

END PROJECT 6054-05-75
 STA 690+81

STA 650+57 - STA 651+33
 STA 646+70 - STA 647+30
 STA 643+12 - STA 644+22
 EXCEPTION TO NET LENGTH OF C/L

BEGIN PROJECT 6054-05-75
 STA 614+50

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	NC REGION
Designer	REBECCA OLSEN
Project Manager	DAN HOLLOWAY
Regional Examiner	CHERYL SIMON
Regional Supervisor	NICHOLE LYSNE

APPROVED FOR THE DEPARTMENT

DATE: 3/19/18 *Dan Holloway*
 (Signature)

GENERAL NOTES

PAVEMENT REMOVAL LIMITS WILL BE AS INDICATED IN THE PLAN OR AS DIRECTED BY THE ENGINEER.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

TOPSOIL SHALL BE PLACED 1" BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS.

CONSTRUCT INSIDE EDGE OF SIDEWALK 0.04' HIGHER THAN TOP OF CURB WHEN THEY ARE ADJACENT TO EACH OTHER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE ENGINEER MAY ADJUST THE LOCATIONS OF ITEMS UNDER THIS CONTRACT TO AVOID CONFLICT WITH THE EXISTING UTILITY FACILITIES

PURSUANT TO CHAPTER 59 OF THE WISCONSIN STATUTES, THE CONTRACTOR SHALL CAREFULLY MAKE A SEARCH FOR EVIDENCE OF A LANDMARK IN ALL AREAS WHERE SUCH A LANDMARK MAY EXIST.

WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. ONLY REMOVE SURVEY MARKERS WITH THE APPROVAL OF THE ENGINEER

DNR

DNR NORTHEAST REGIONAL HEADQUARTERS
2984 SHAWANO AVE
GREEN BAY, WI 54313
PHONE: (920)662-5130

UTILITIES

ADAMS-COLUMBIA ELECTRIC COOPERATIVE - ELECTRICITY

DUANE MOORE - OPERATIONS SUPERVISOR WAUTOMA
P O BOX 70
FRIENDSHIP WI 53934
PHONE: (800) 831-8629 EXT 424
E-MAIL: dmoore@acecwi.com

WE ENERGIES - GAS/PETROLEUM

JACOB HULBERT
1921 8TH STREET SOUTH
WISCONSIN RAPIDS WI 54494
PHONE: (715)421-7277
MOBILE: (715)213-5189
E-MAIL: jacob.hulbert@we-energies.com

CITY OF MONTELLO - WATER & SEWER

MICHAEL KOHNKE
20 UNDERWOOD AVE
MONTELLO WI 53949
PHONE: (608) 297-2416
MOBILE: (608)697-5970
E-MAIL: pmdir.kohnke@cityofmontello.com

ALLIANT ENERGY - ELECTRICITY

MATT SCHMITZ, ENGINEERING TECHNICIAN
506 FENTON STREET
RIPON WI 54971
PHONE: (920)748-4011
MOBILE: (920)238-1137
E-MAIL: matthewschmitz@alliantenergy.com


CHARTER COMMUNICATIONS - COMMUNICATION LINE

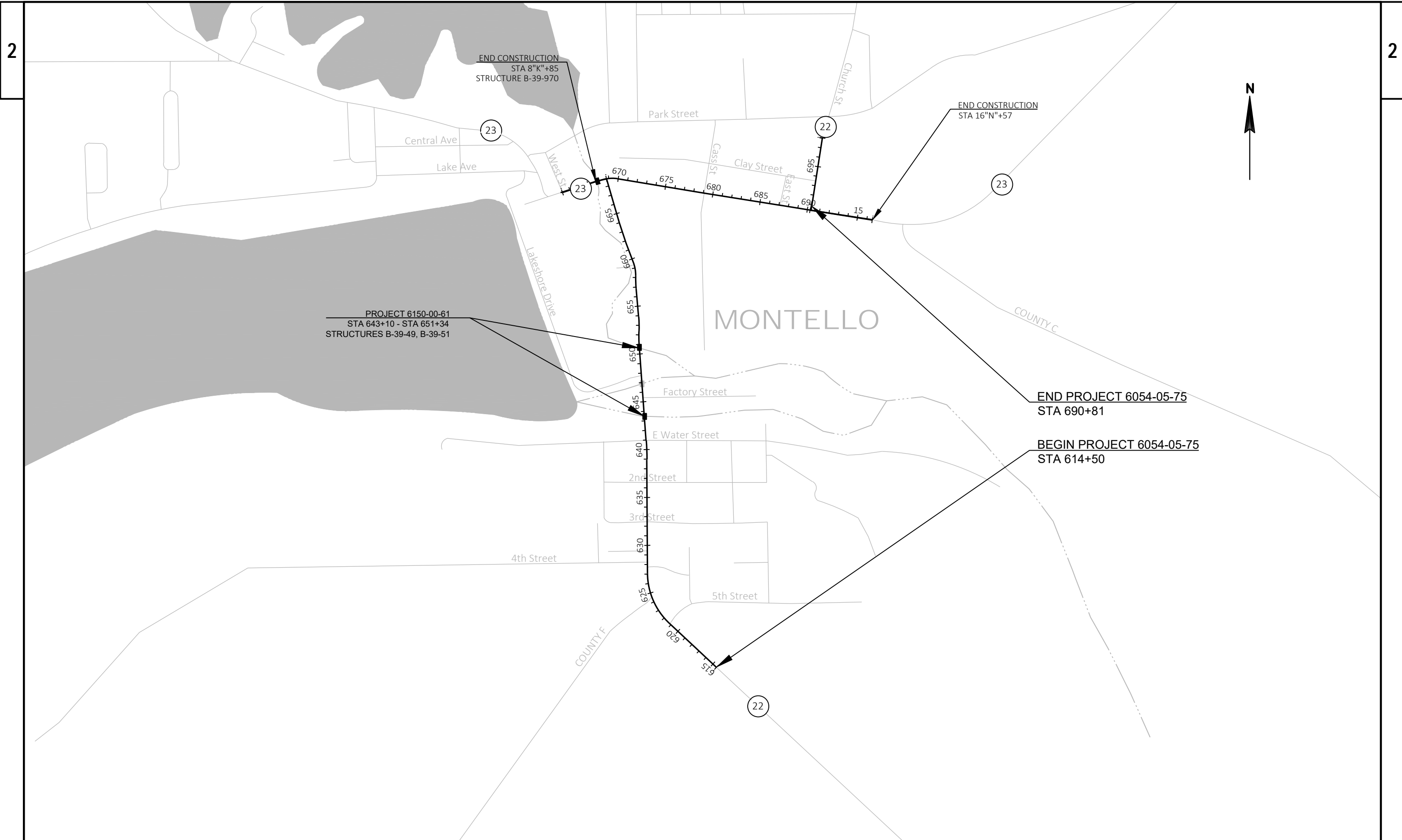
NICK FRASE
N3760 COUNTY ROAD DJ
JUNEAU WI 53039
PHONE: (920) 304-6797
EMAIL: nick.frase@charter.com

FRONTIER COMMUNICATIONS OF WI LLC - COMMUNICATION LINE

JERRY MOORE
2222 WEST WISCONSIN STREET
PORTAGE WI 53901
PHONE: (608) 742-9507
MOBILE: (608)346-0353
E-MAIL: jerald.r.moore@ftr.com



Dial  or (800)242-8511
www.DiggersHotline.com



2

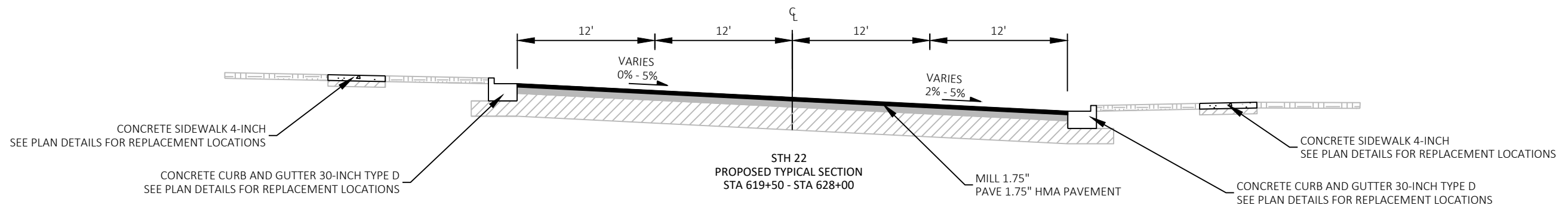
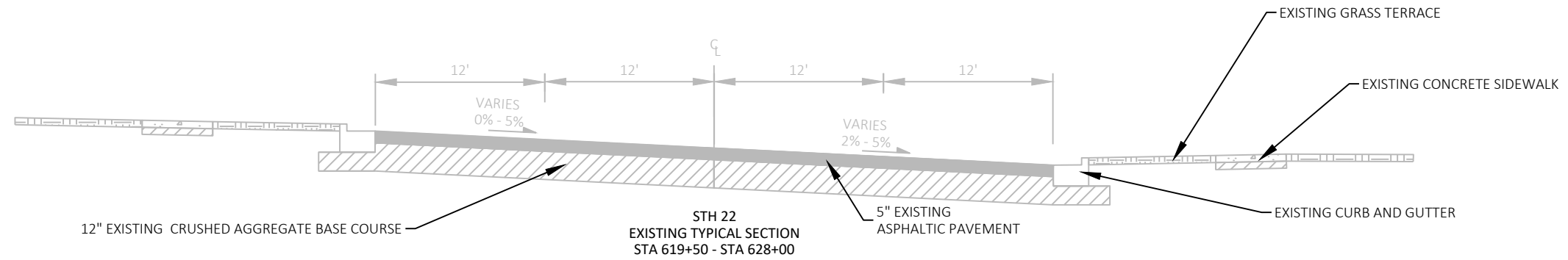
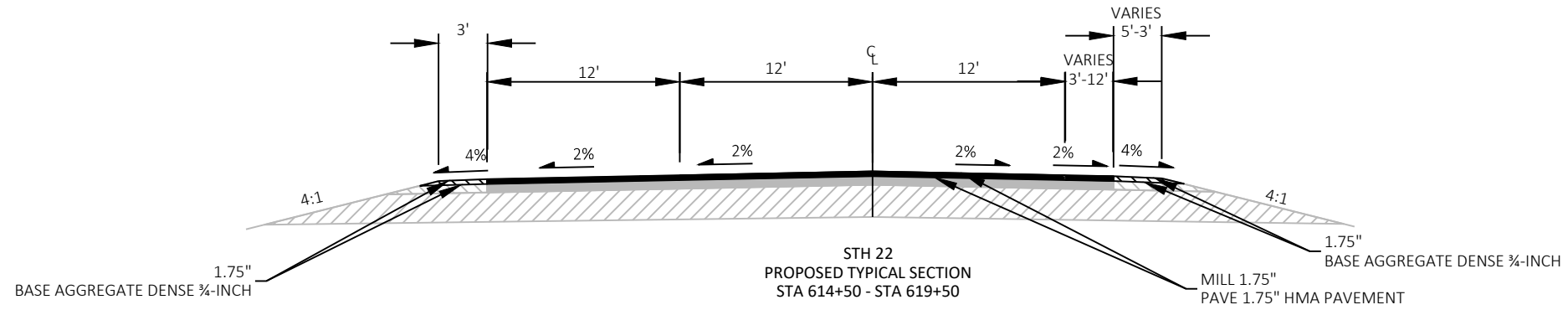
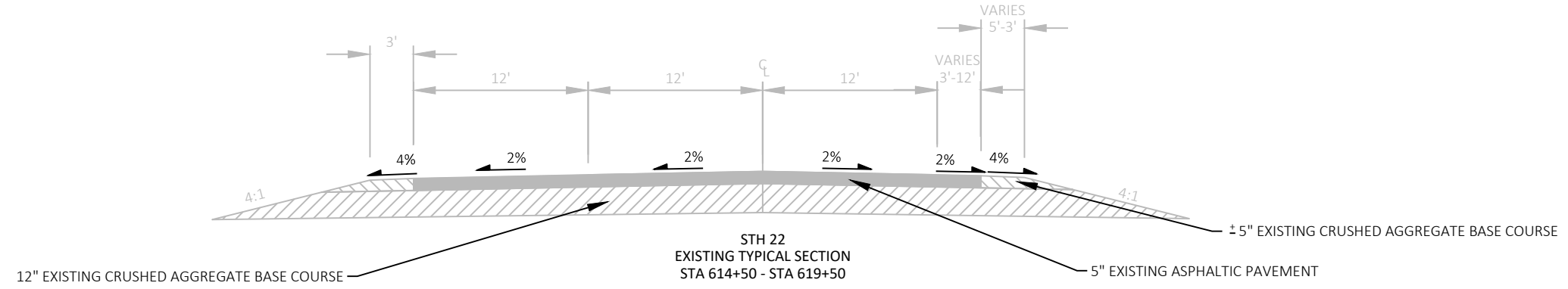
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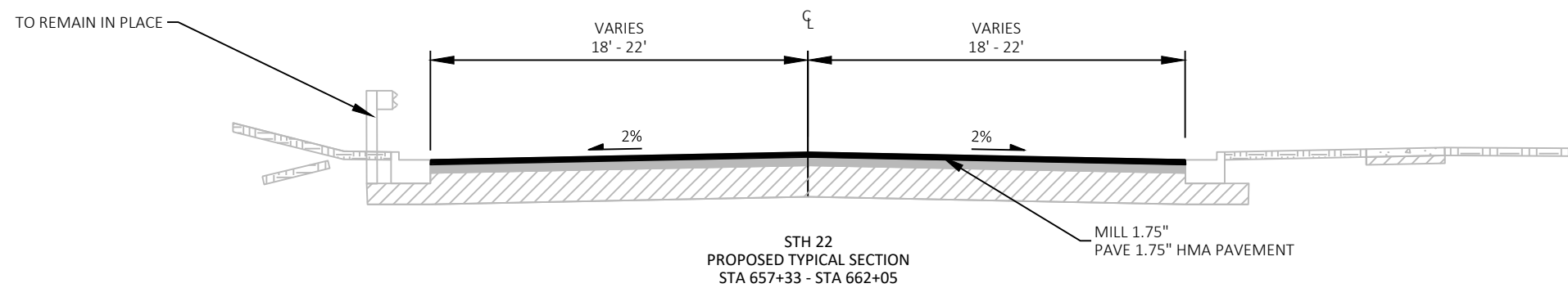
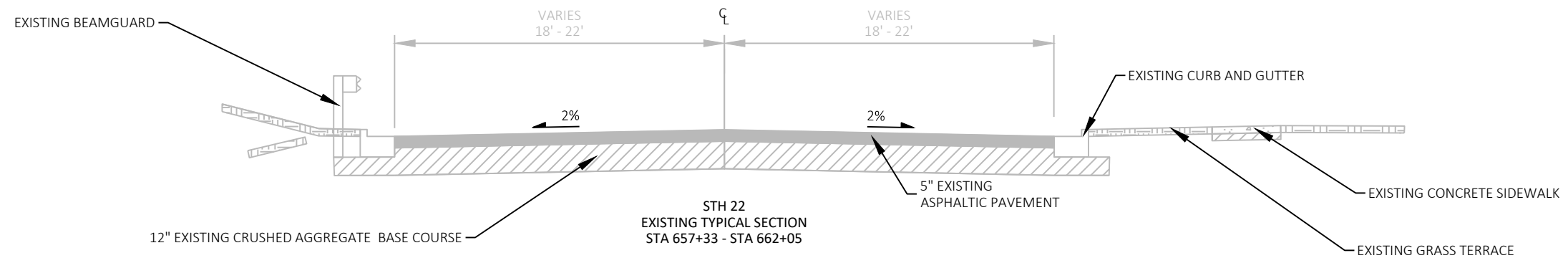
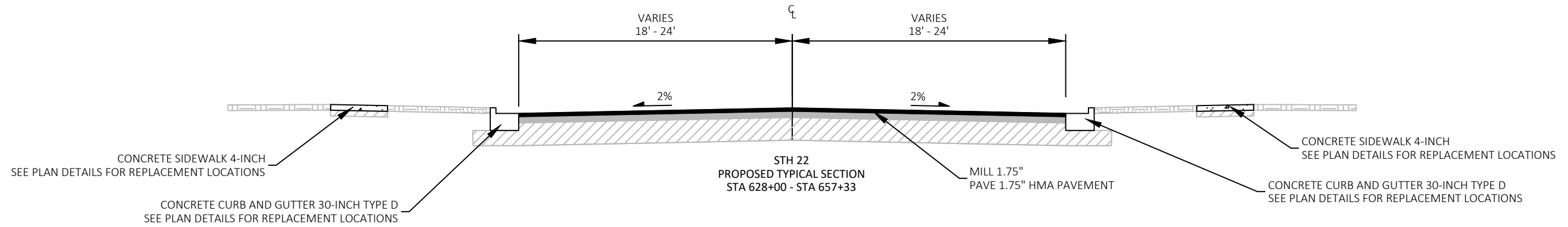
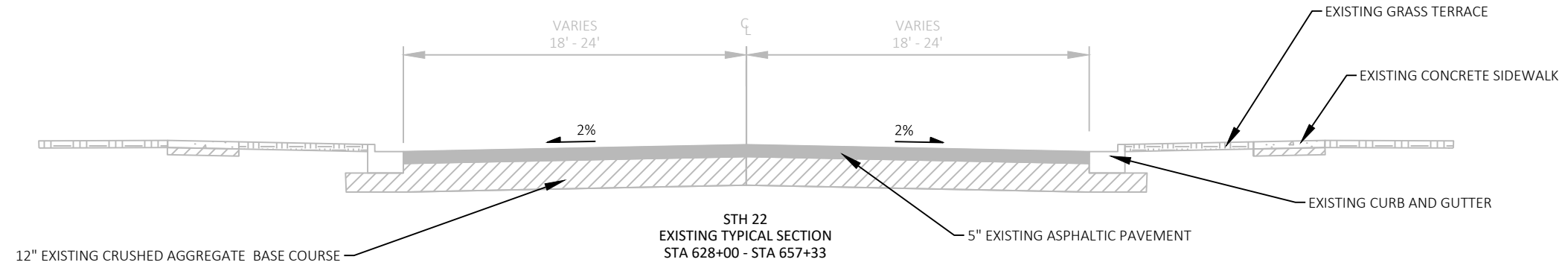


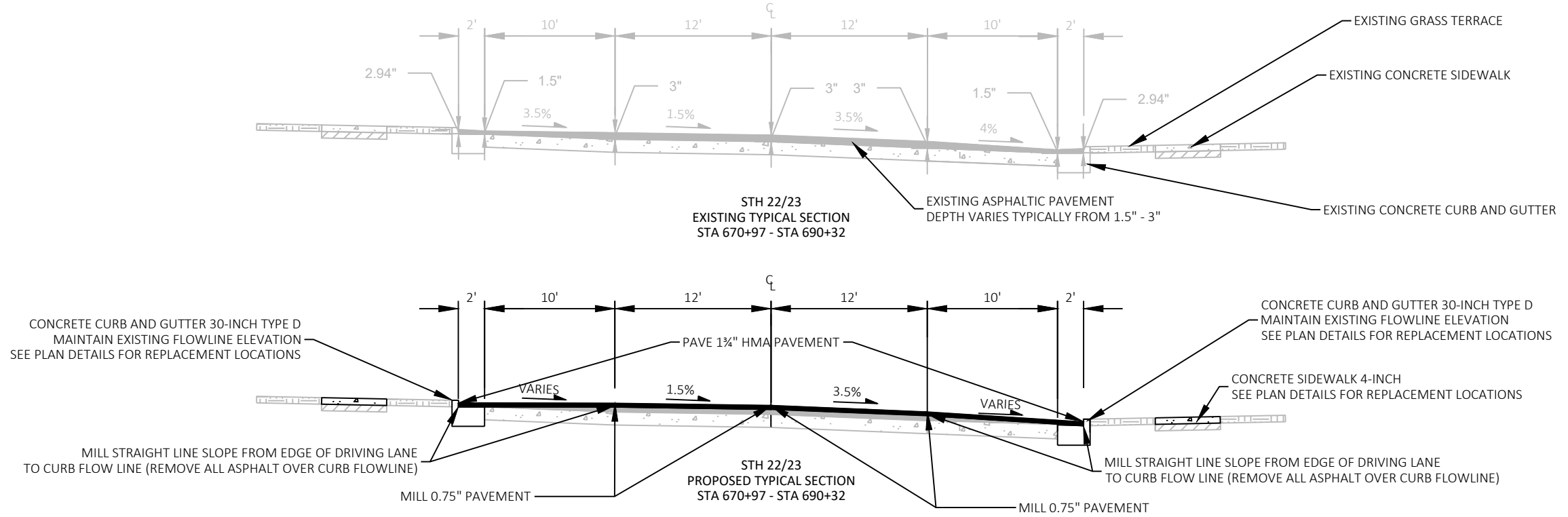
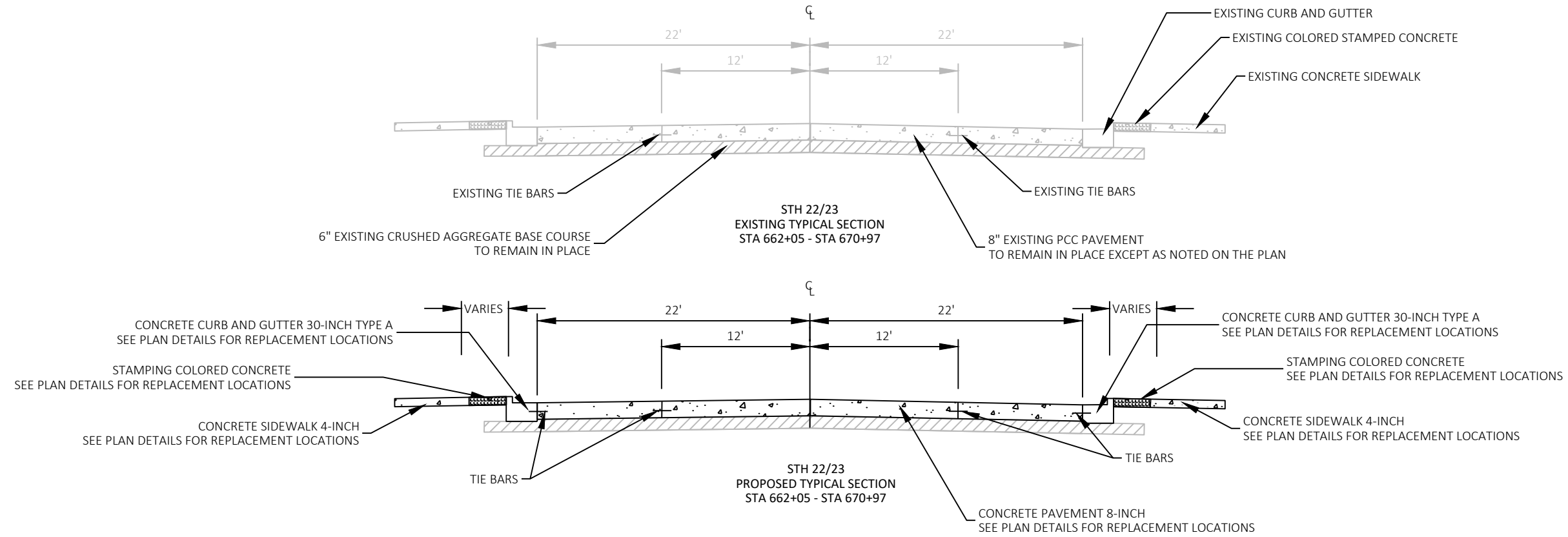
PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	PROJECT OVERVIEW	SHEET	E
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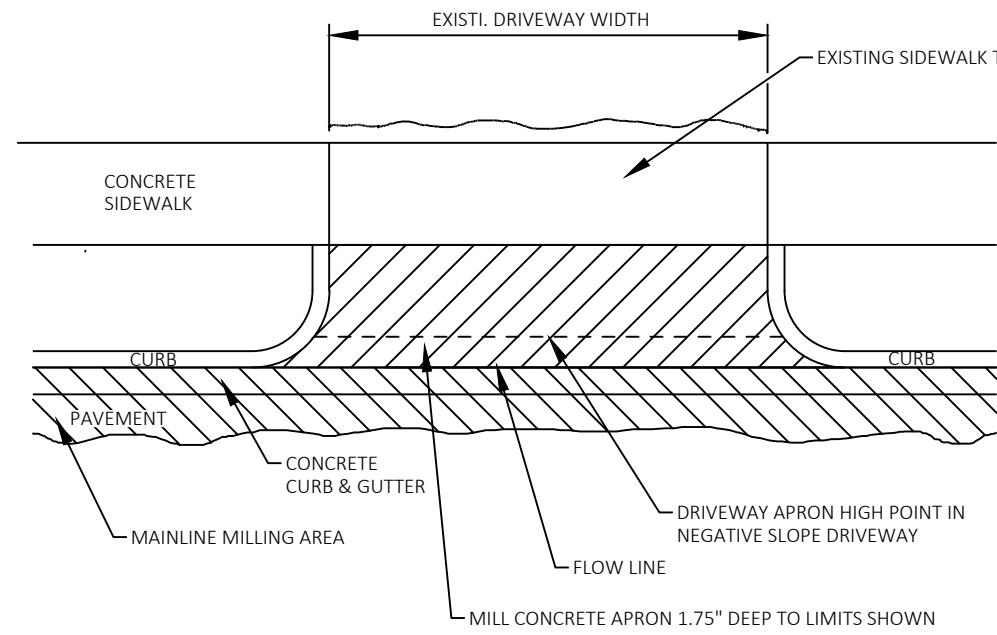
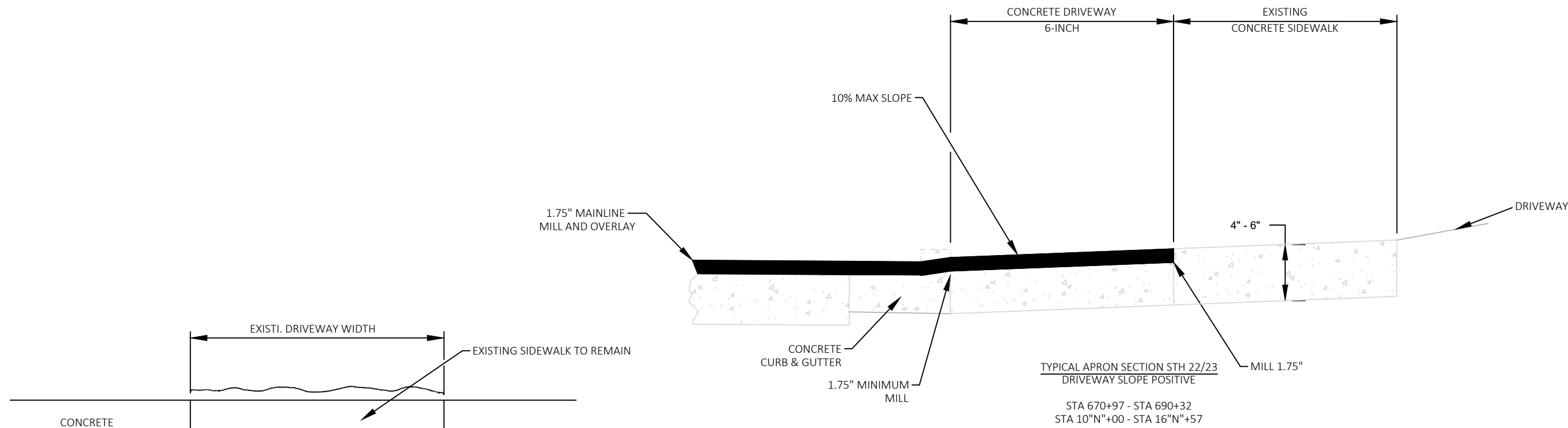
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LAYOUT NAME - Plan 1 IN 100 FT

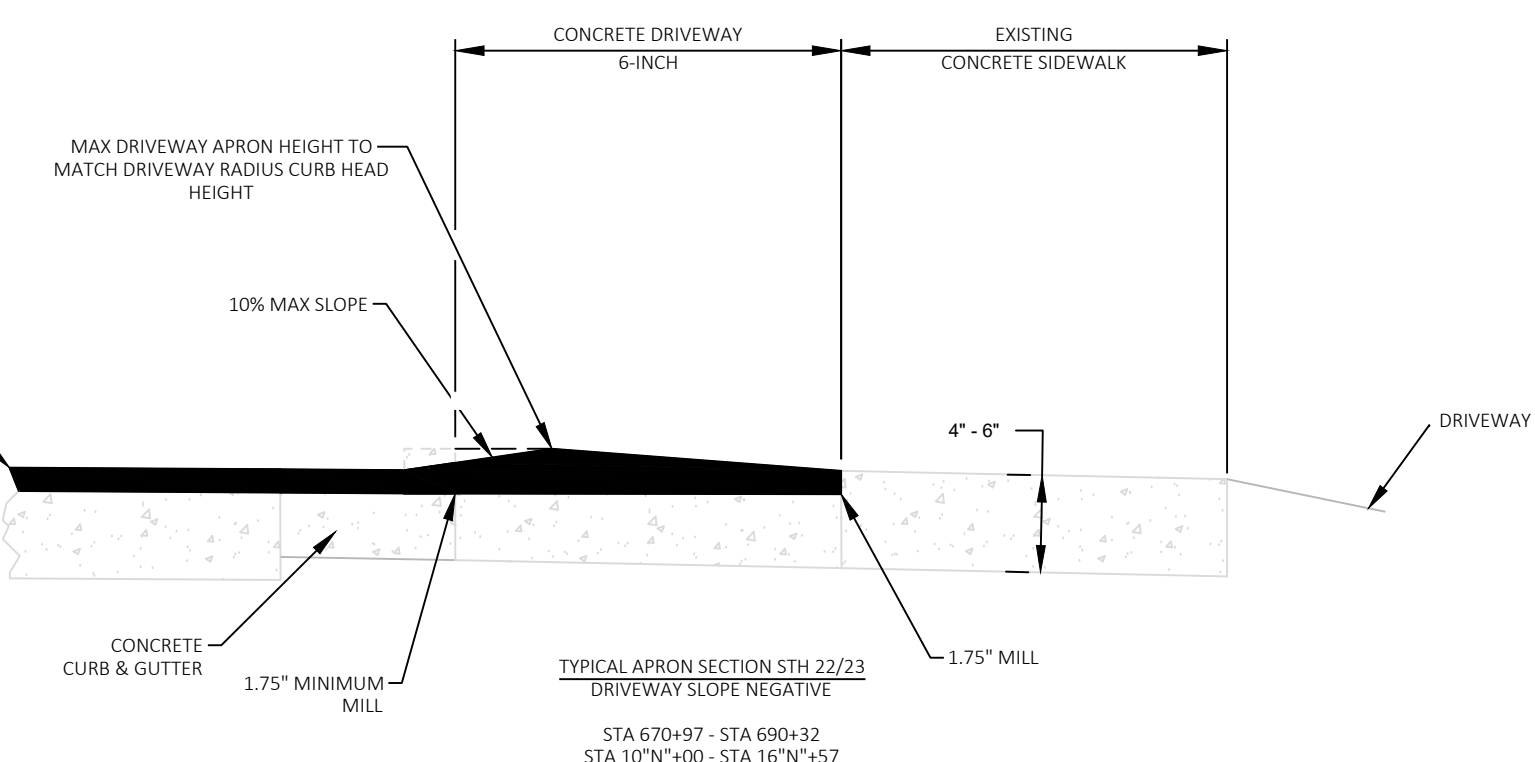




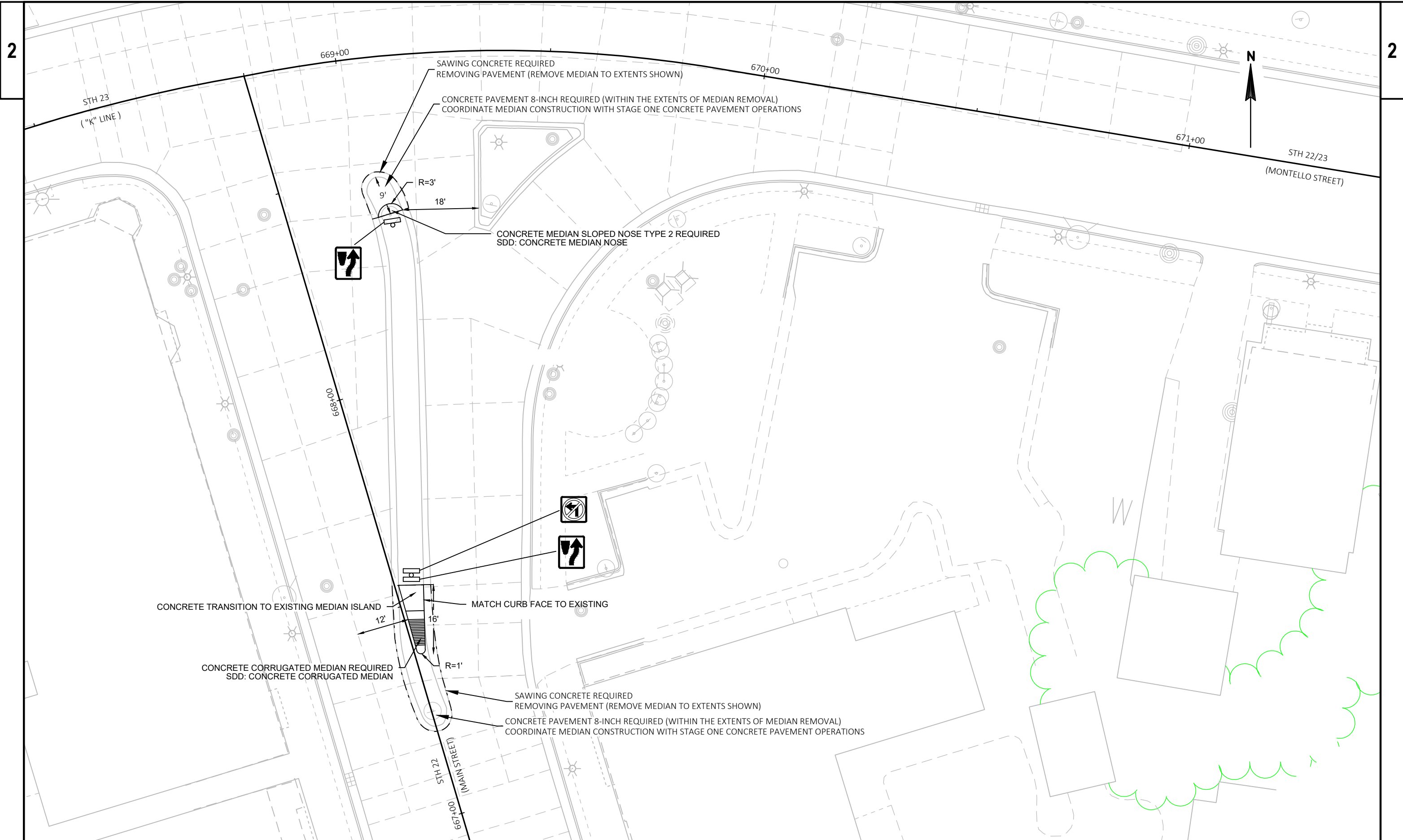




PLAN VIEW STH 22/23
 STA 670+97 - STA 690+32
 STA 10"N"+00 - STA 16"N"+57



TYPICAL APRON SECTION STH 22/23
 DRIVEWAY SLOPE NEGATIVE
 STA 670+97 - STA 690+32
 STA 10"N"+00 - STA 16"N"+57



PROJECT NO: 6054-05-75

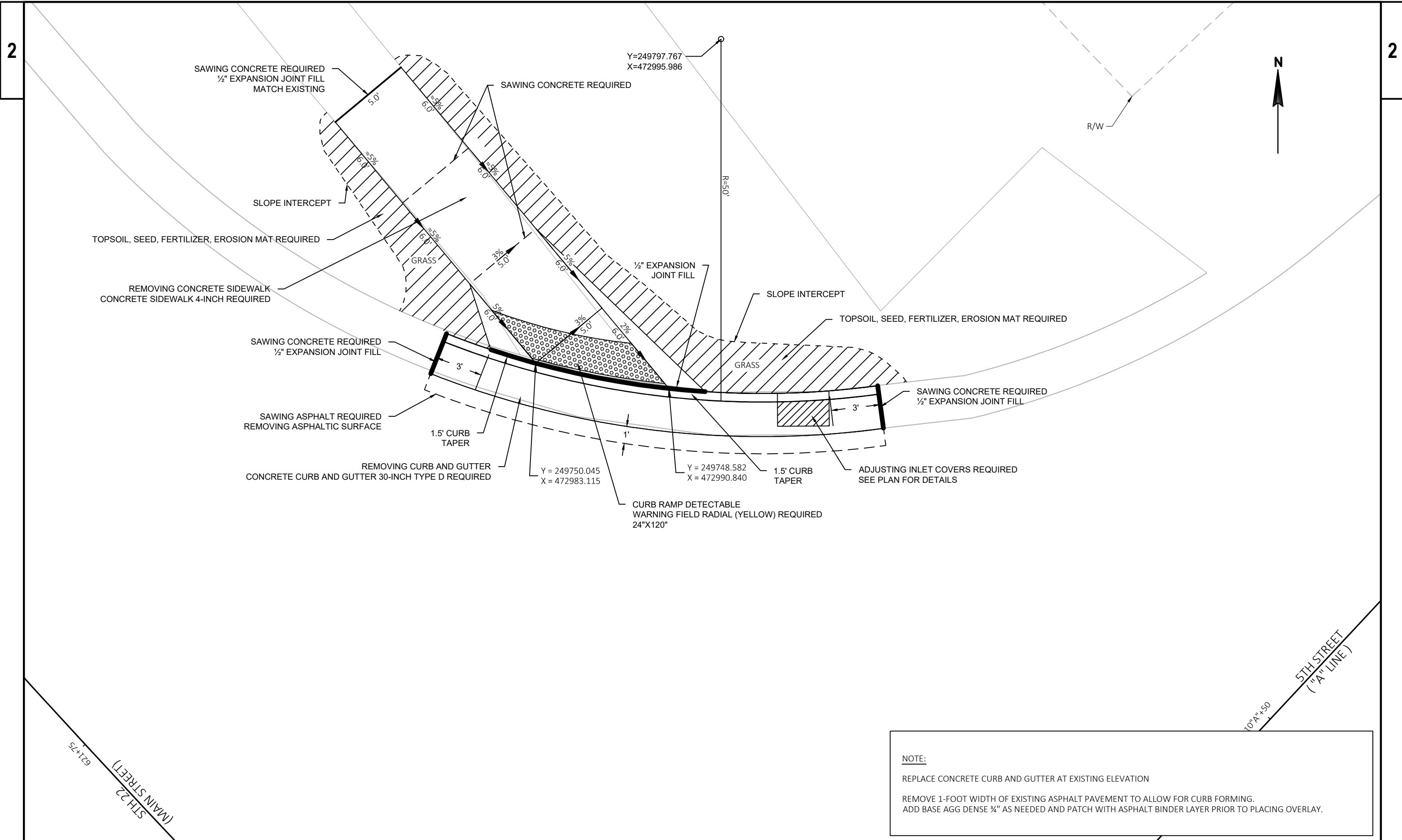
HWY: STH 22

COUNTY: MARQUETTE

CONSTRUCTION DETAIL - MEDIAN NOSE

SHEET

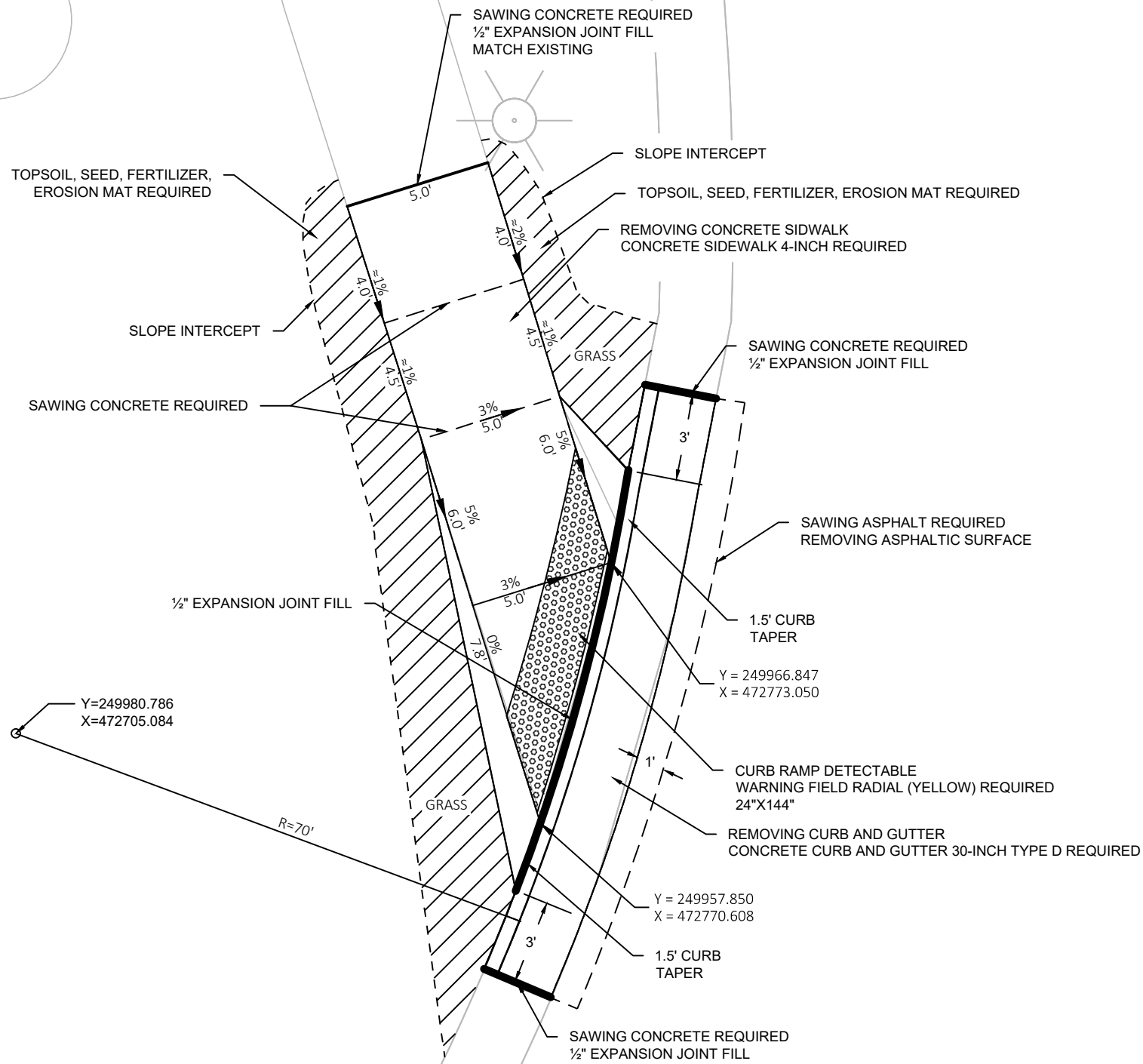
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NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
 ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

2

2



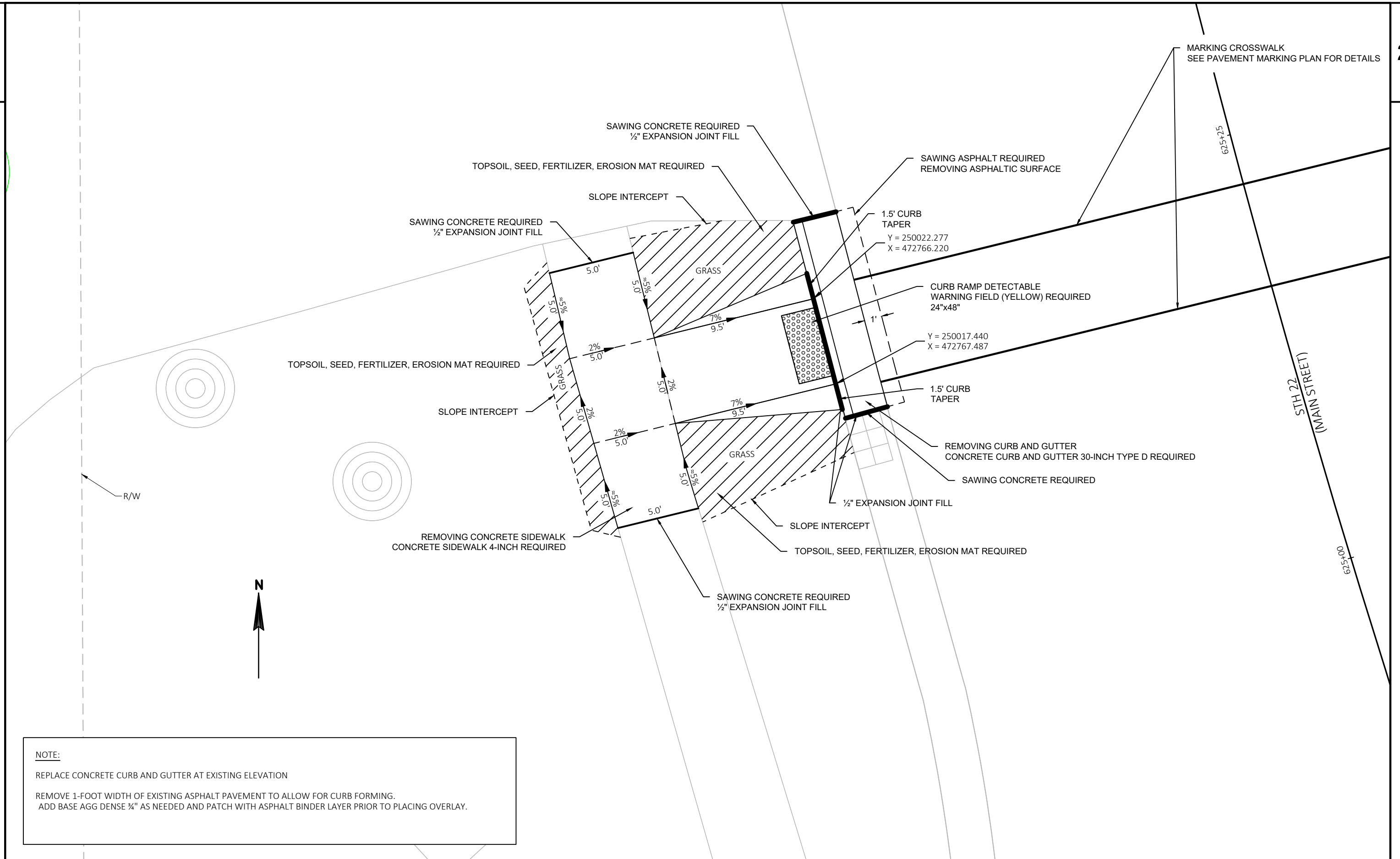
624+75
STH 22 (MAIN STREET)

624+50

STA 624+44
STA 10"B"+00

COUNTY F
("B" LINE)

NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
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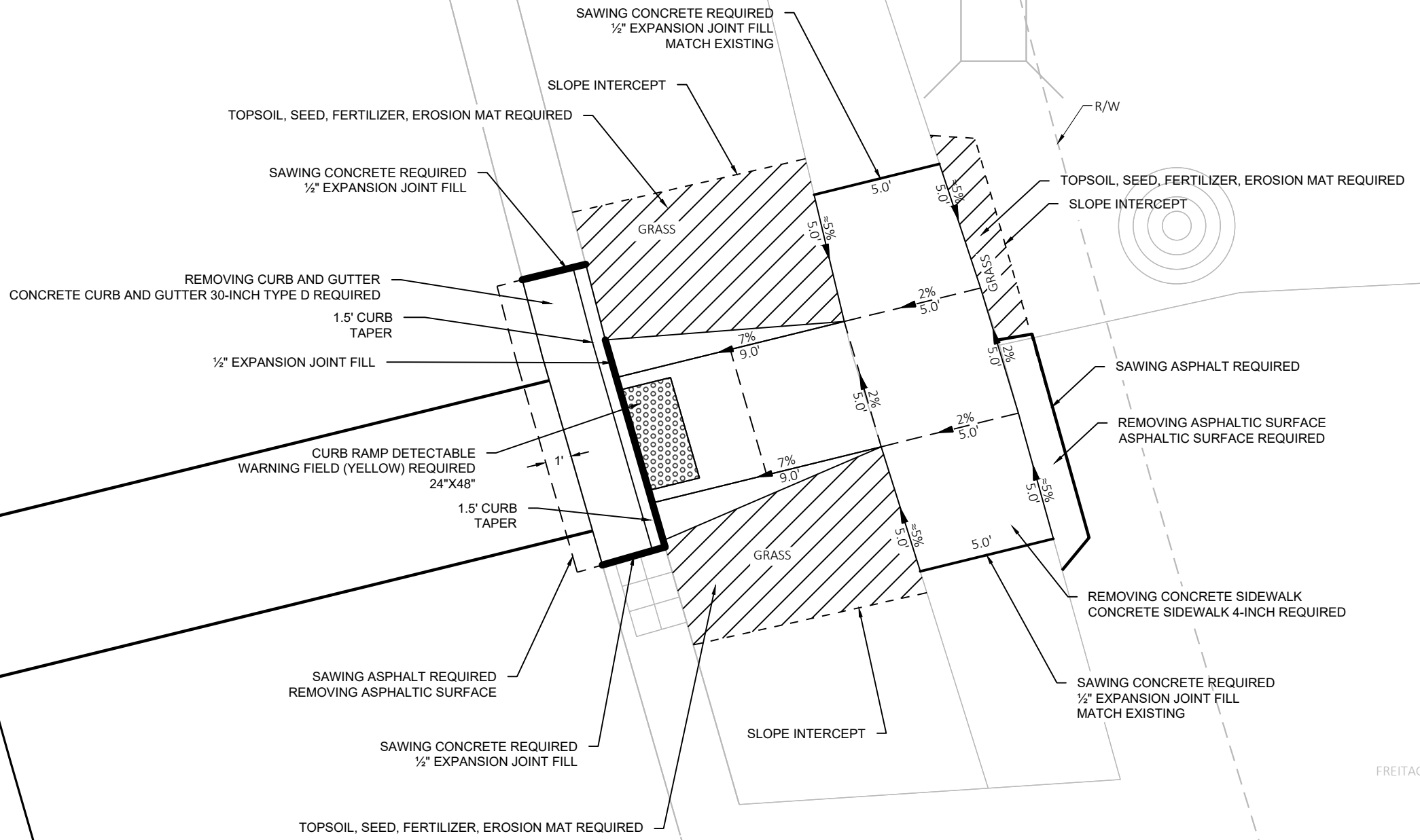


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STH 22
(MAIN STREET)

625+75



MARKING CROSSWALK
SEE PAVEMENT MARKING PLAN FOR DETAILS

NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
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 ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

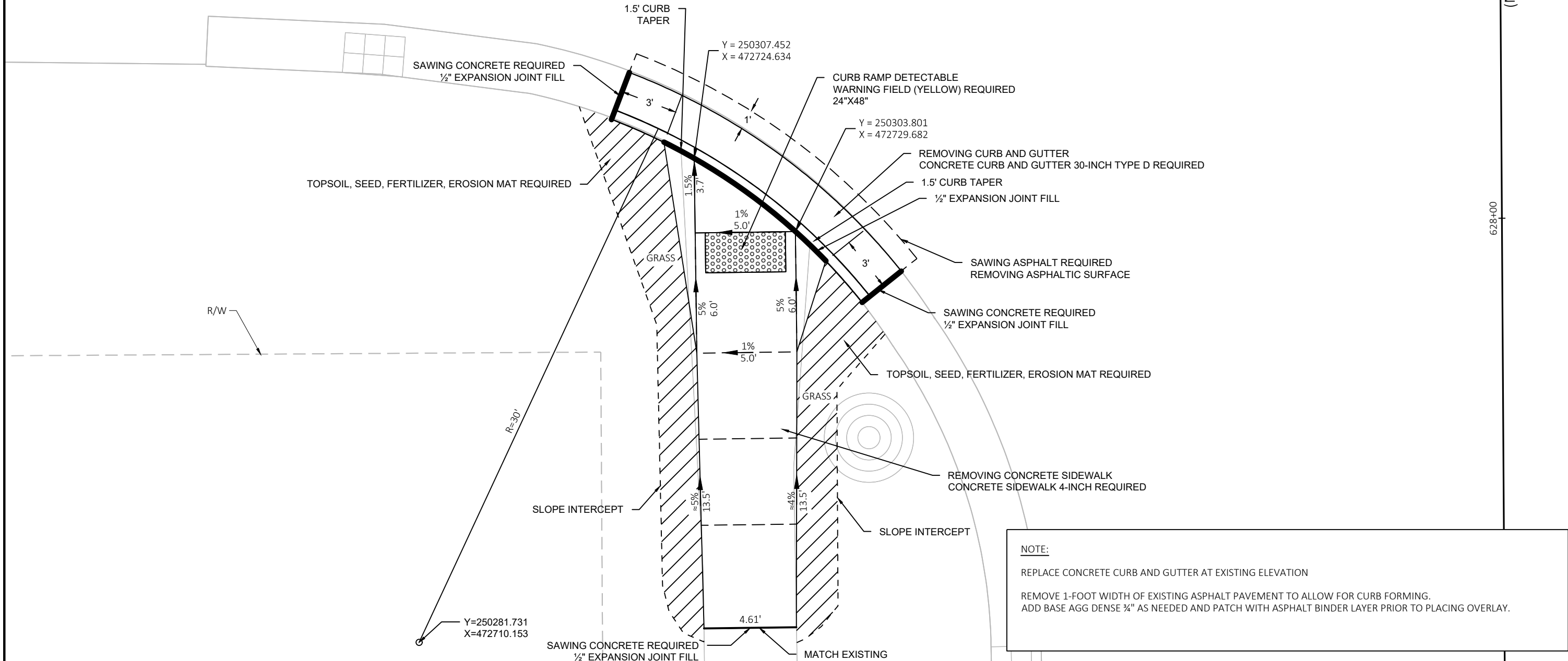
FREITAG'S FOOD CENTER

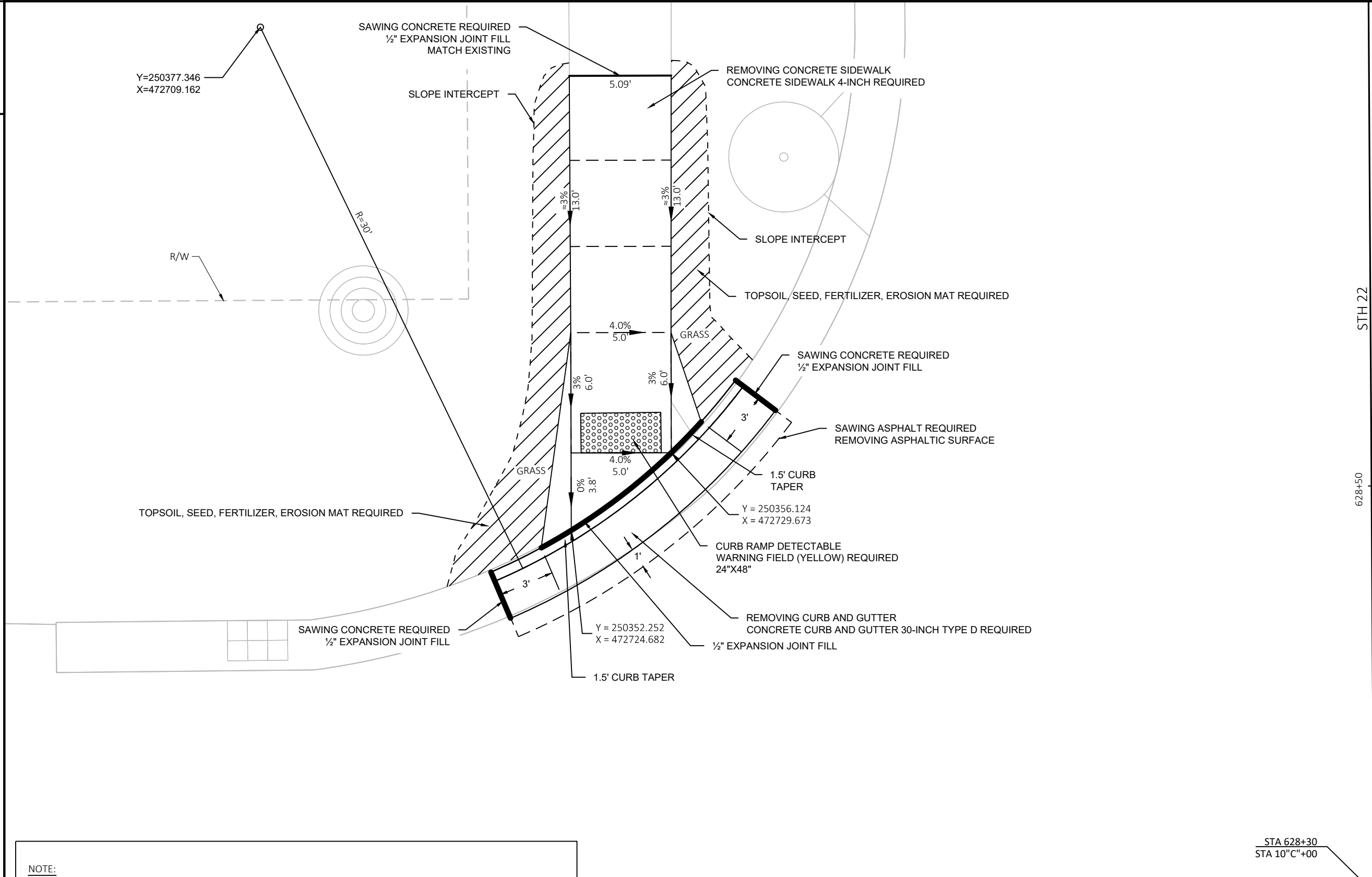


628+25

STH 22
(MAIN STREET)

628+00





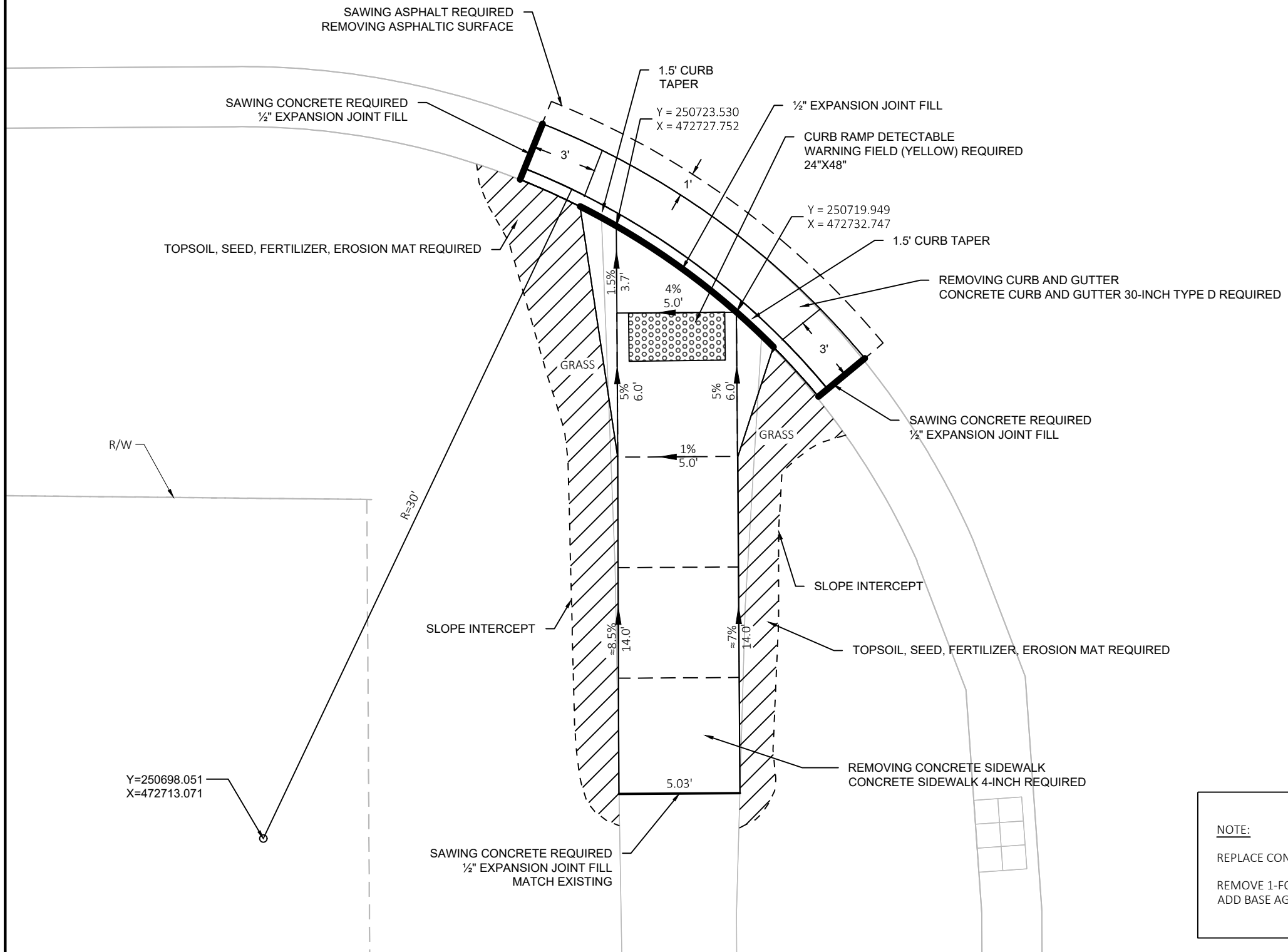
STH 22
(MAIN STREET)
628+50

STA 628+30
STA 10\"C\"+00

4TH STREET
("C" LINE)

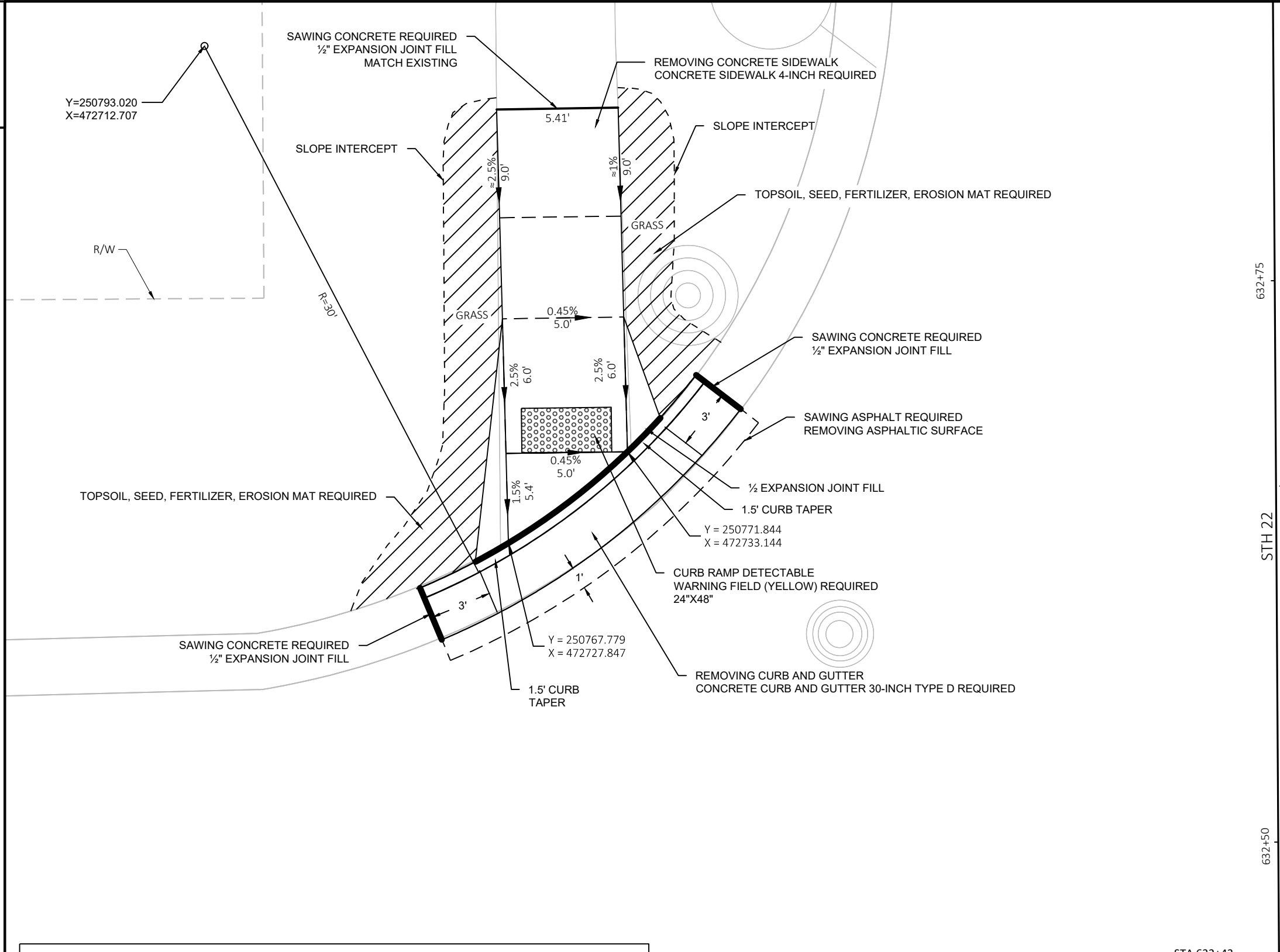
NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
 ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

3RD STREET
("D" LINE)



632+25
STH 22
(MAIN STREET)
632+00

NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
 ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

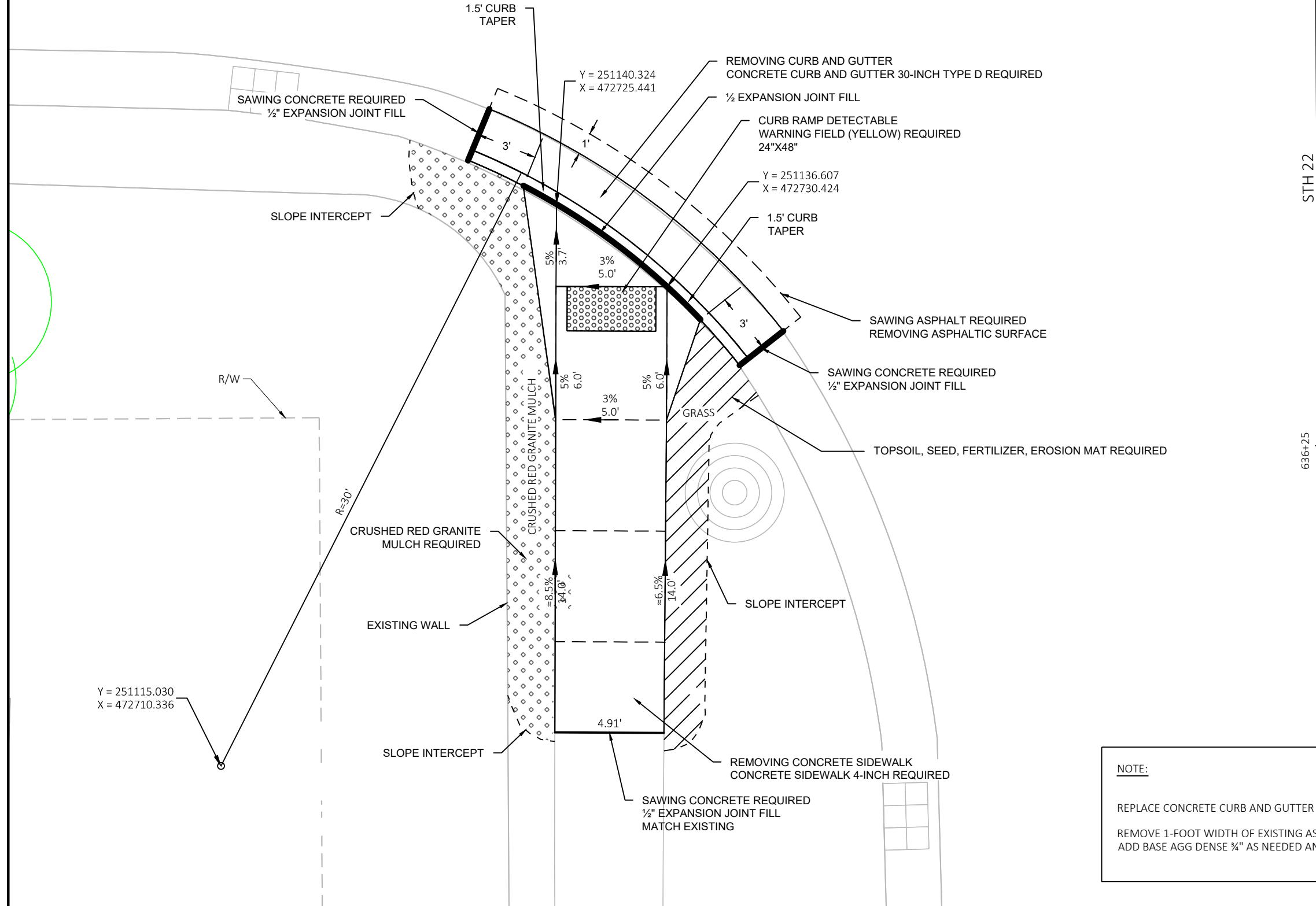


NOTE:

REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION

REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
ADD BASE AGG DENSE ¾" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

2ND STREET
("E" LINE)



NOTE:

REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION

REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
ADD BASE AGG DENSE ¾" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

2ND STREET
("E" LINE)



STH 22
(MAIN STREET)

636+25

MARKING CROSSWALK
SEE PAVEMENT MARKING PLAN FOR DETAILS

CURB RAMP DETECTABLE
WARNING FIELD (YELLOW) REQUIRED
24"X48"

Y = 251140.123
X = 472795.247

1.5' CURB
TAPER

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

Y = 251137.160
X = 472790.272

1.5' CURB TAPER

1/2" EXPANSION JOINT FILL

REMOVING CURB AND GUTTER
CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED

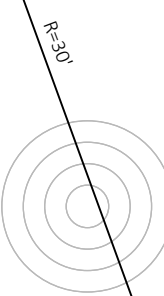
SAWING ASPHALT REQUIRED
REMOVING ASPHALTIC SURFACE

TOPSOIL, SEED, FERTILIZER, EROSION MAT REQUIRED

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

GRASS

GRASS



R/W

TOPSOIL, SEED, FERTILIZER, EROSION MAT REQUIRED

SLOPE INTERCEPT

Y = 251117.981
X = 472805.643

REMOVING CONCRETE SIDEWALK
CONCRETE SIDEWALK 4-INCH REQUIRED

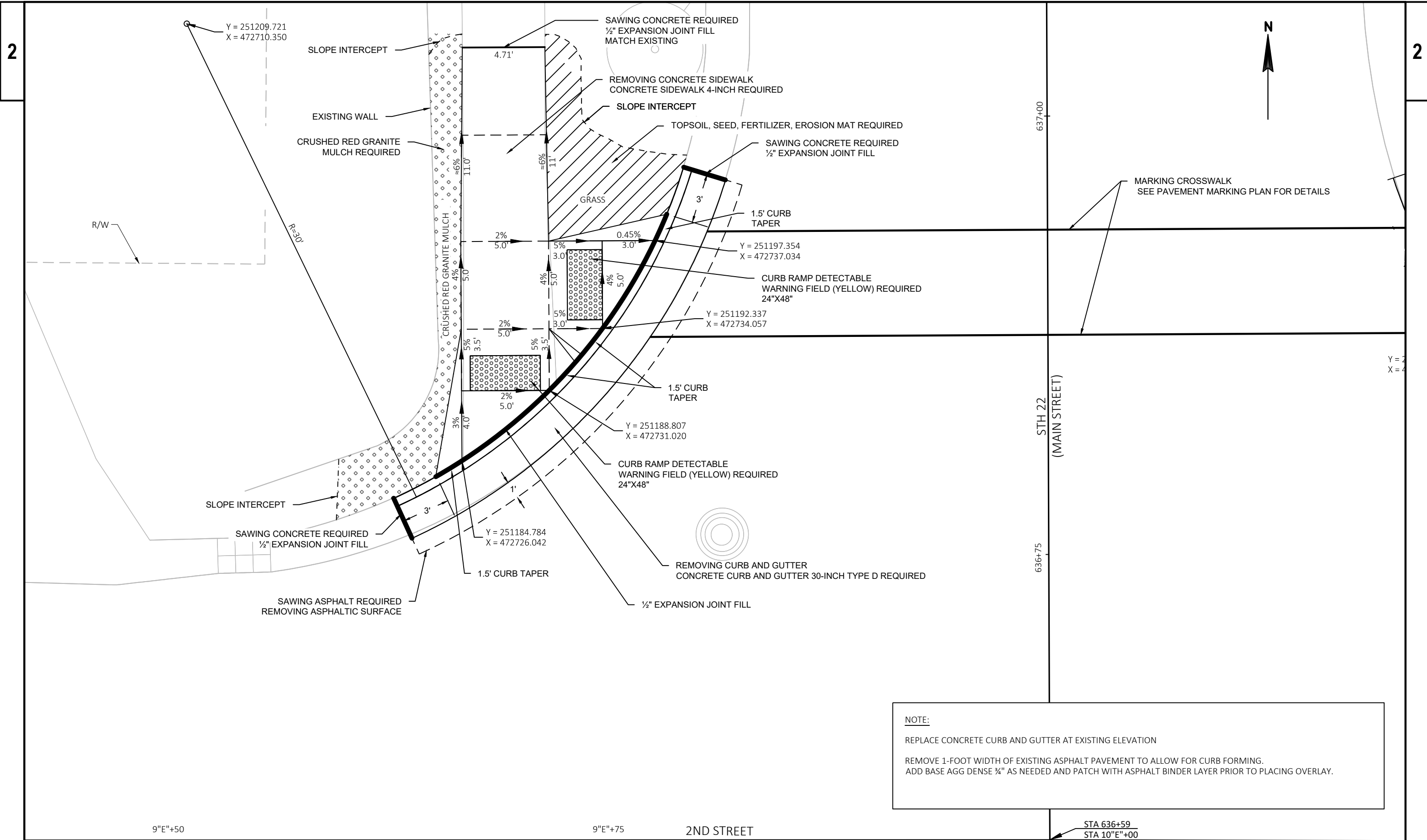
SLOPE INTERCEPT

MATCH EXISTING

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

NOTE:

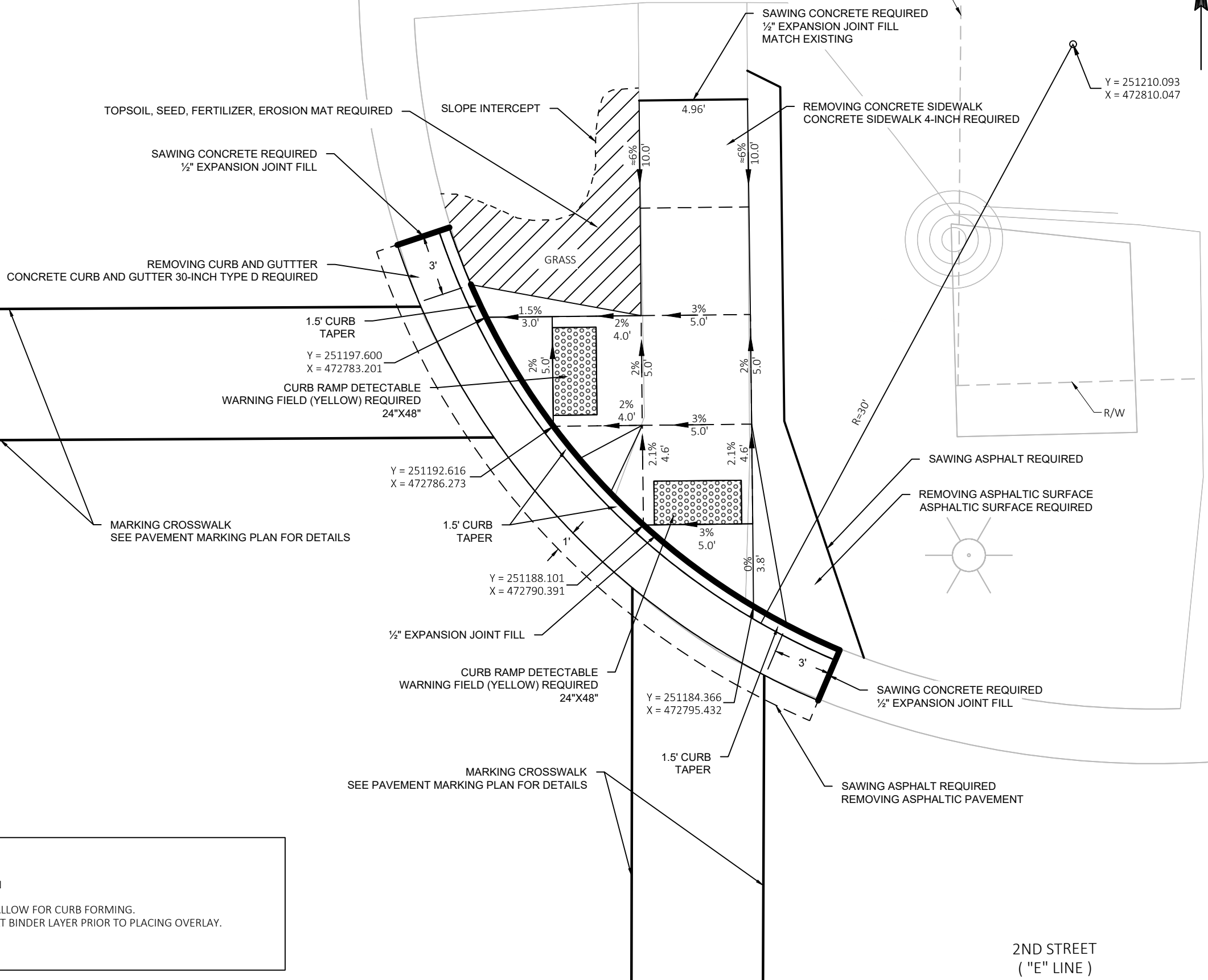
REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.



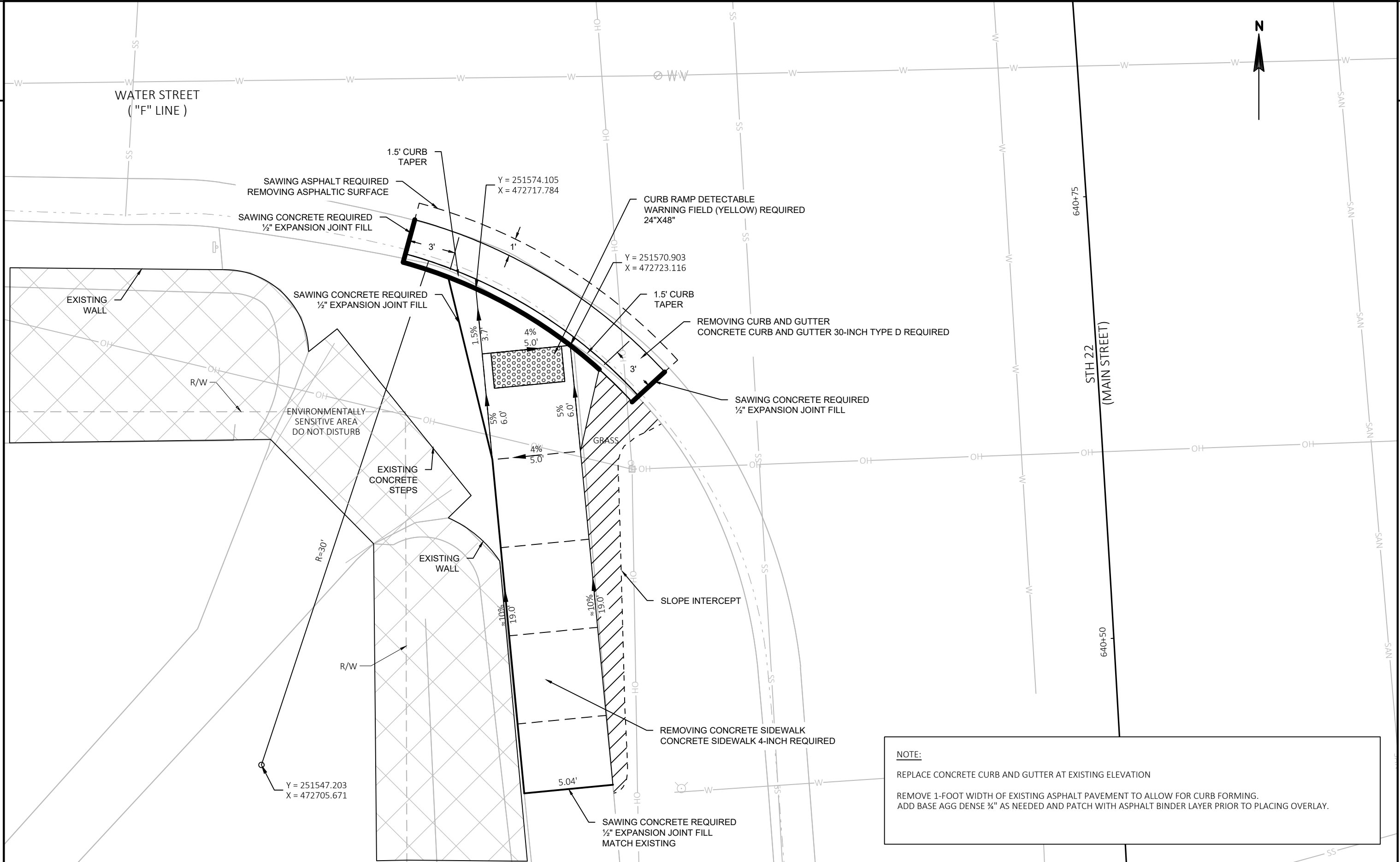
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WATER STREET ("F" LINE)



640+75

STH 22 (MAIN STREET)

640+50

NVS SAN

1/2" EXPANSION JOINT FILL

CURB RAMP DETECTABLE WARNING FIELD (YELLOW) REQUIRED 24"X48"

Y = 251575.811
X = 472788.450

SAWING CONCRETE REQUIRED 1/2" EXPANSION JOINT FILL

SLOPE INTERCEPT

Y = 251572.080
X = 472783.638

1.5' CURB TAPER

REMOVING CURB AND GUTTER CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED

SAWING ASPHALT REQUIRED REMOVING ASPHALTIC SURFACE

SAWING CONCRETE REQUIRED 1/2" EXPANSION JOINT FILL

3.5% 5.0'

5%

5%

3.5%

5%

GRASS

TOPSOIL, SEED, FERTILIZER, EROSION MAT REQUIRED

SLOPE INTERCEPT

3'

5.0'

6.0'

20.0'

4.94'

SAWING CONCRETE REQUIRED 1/2" EXPANSION JOINT FILL MATCH EXISTING

CRUSHED RED GRANITE MULCH REQUIRED

EXISTING WALL

REMOVING CONCRETE SIDEWALK CONCRETE SIDEWALK 4-INCH REQUIRED

SLOPE INTERCEPT

R/W

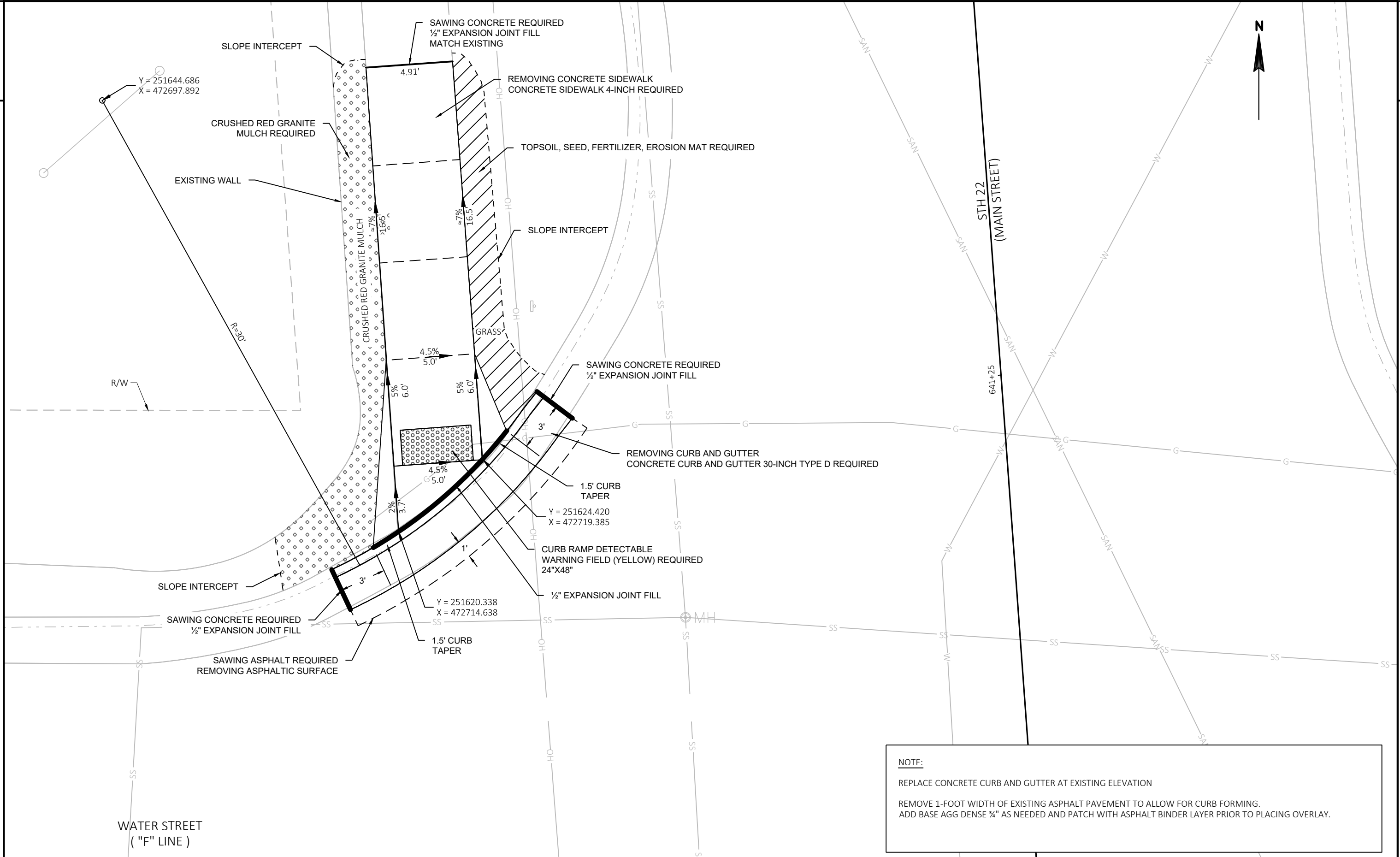
R=30'

Y = 251550.410
X = 472803.518

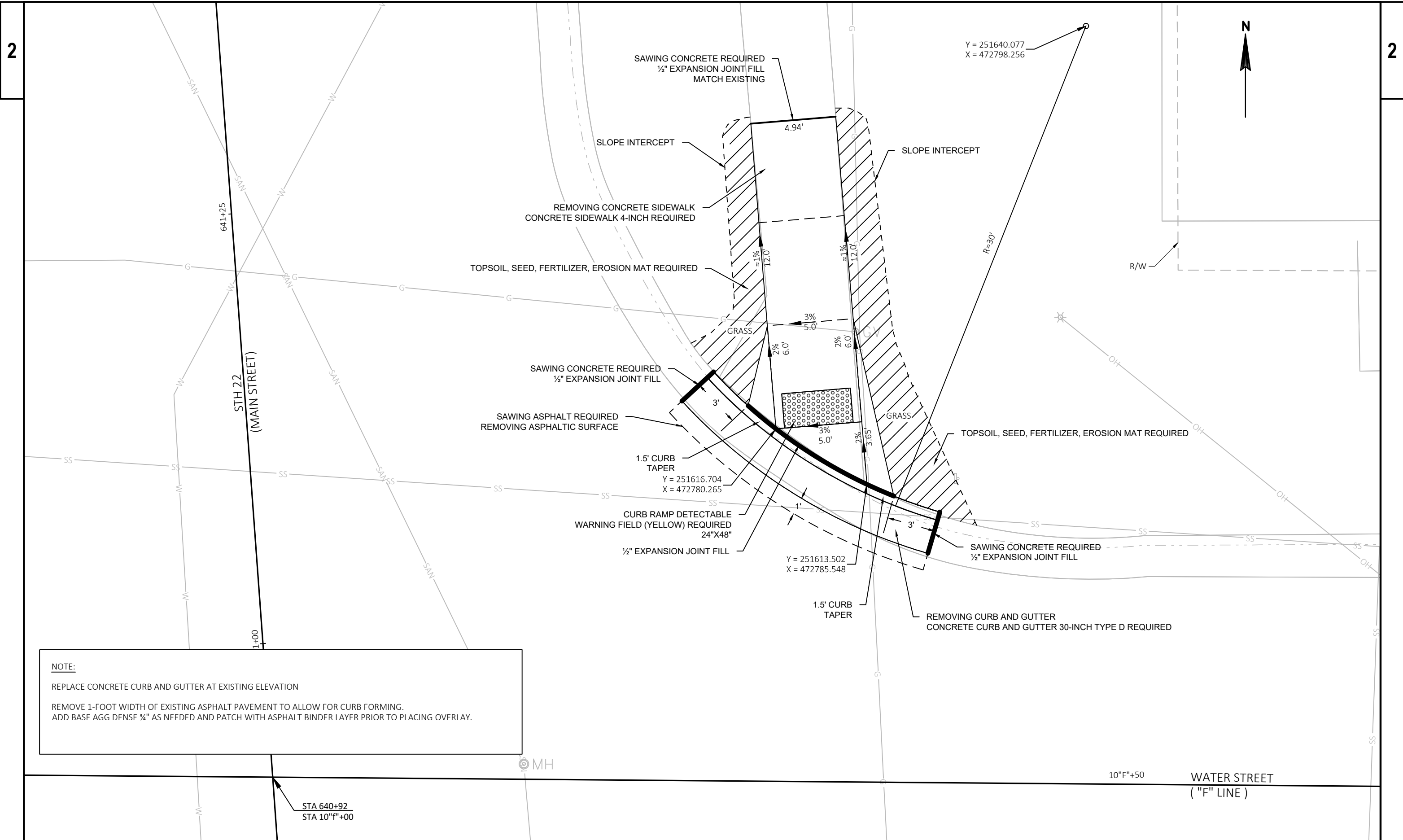
NOTE:

REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION

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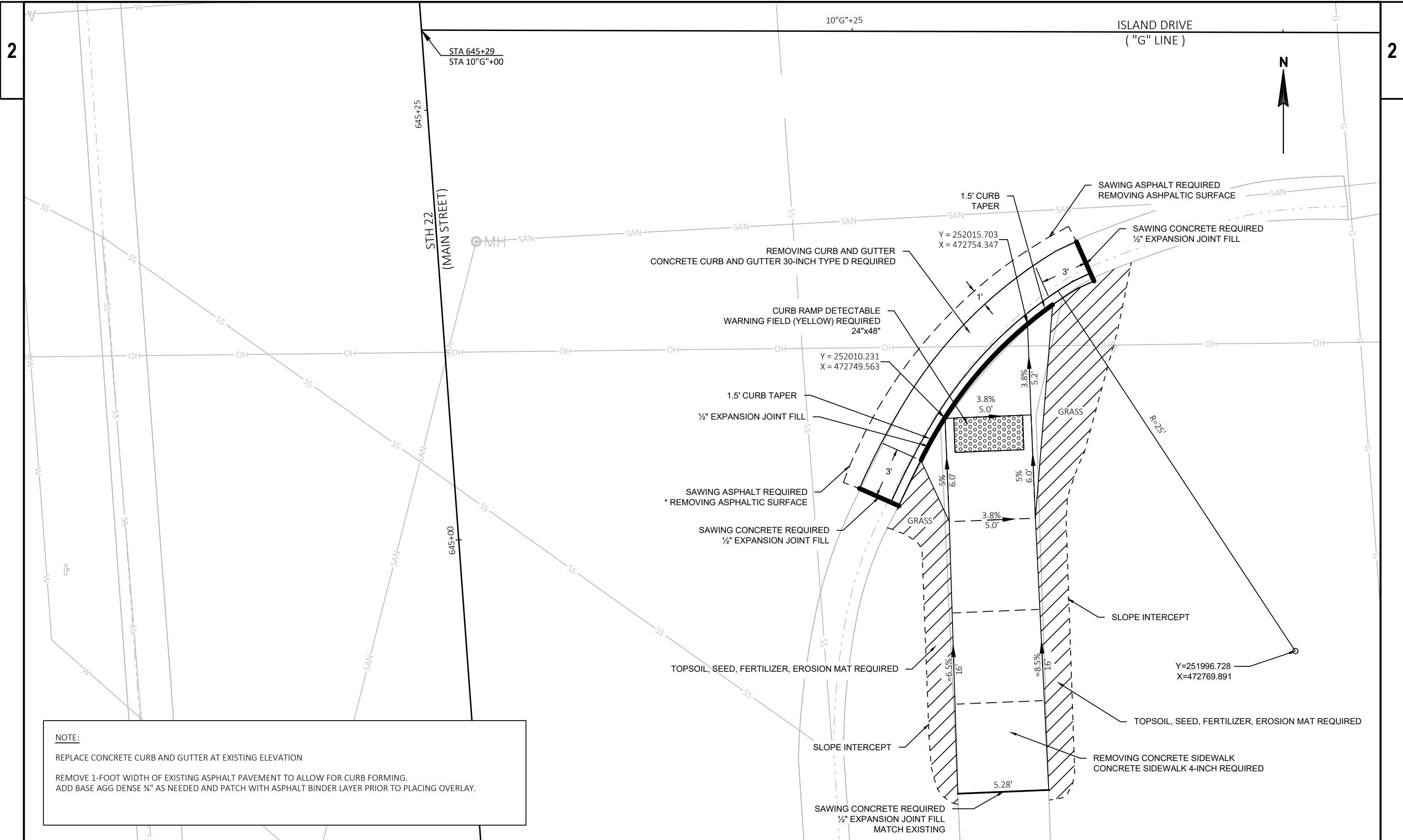


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STA 640+92
 STA 10"F"+00



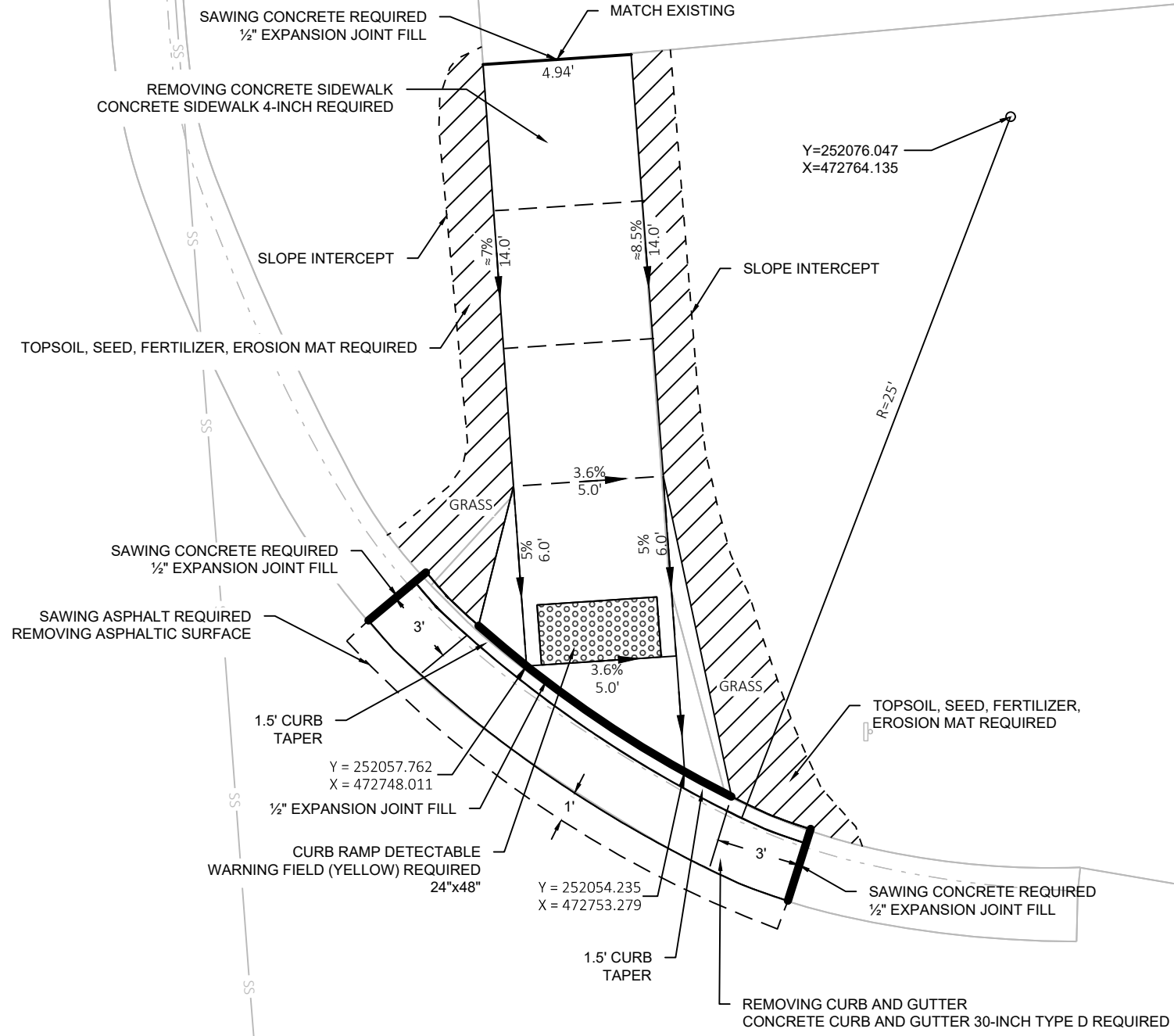
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645+75

STH 22
(MAIN STREET)

645+50



Y=252076.047
X=472764.135

Y = 252057.762
X = 472748.011

Y = 252054.235
X = 472753.279

NOTE:
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STA 645+29
STA 10"G"+00

10"G"+25

ISLAND DRIVE
("G" LINE)

10"G"+50

SUNSET DRIVE
("H" LINE)

RW

SAWING ASPHALT REQUIRED
REMOVING ASPHALTIC SURFACE

1' CURB
TAPER

Y = 252249.185
X = 472669.656

CURB RAMP DETECTABLE
WARNING FIELD (YELLOW) REQUIRED
24"x48"

Y = 252248.040
X = 472674.731

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

1.5' CURB
TAPER

REMOVING CURB AND GUTTER
CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

REMOVING CONCRETE SIDEWALK
CONCRETE SIDEWALK 4-INCH REQUIRED

Y = 252234.751
X = 472668.953

SLOPE INTERCEPT

TOPSOIL, SEED, FERTILIZER, EROSION MAT REQUIRED

MATCH EXISTING

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

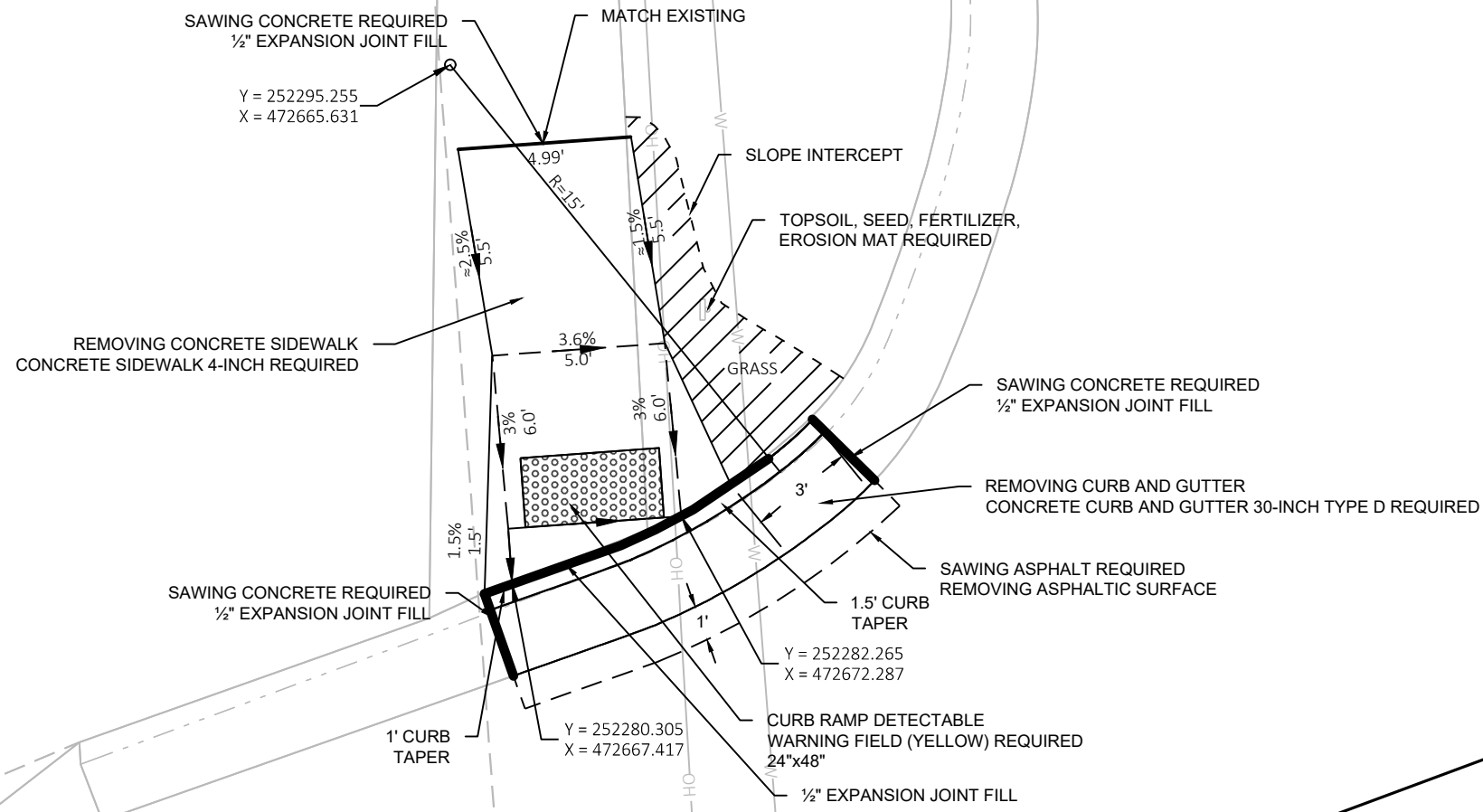


647+50

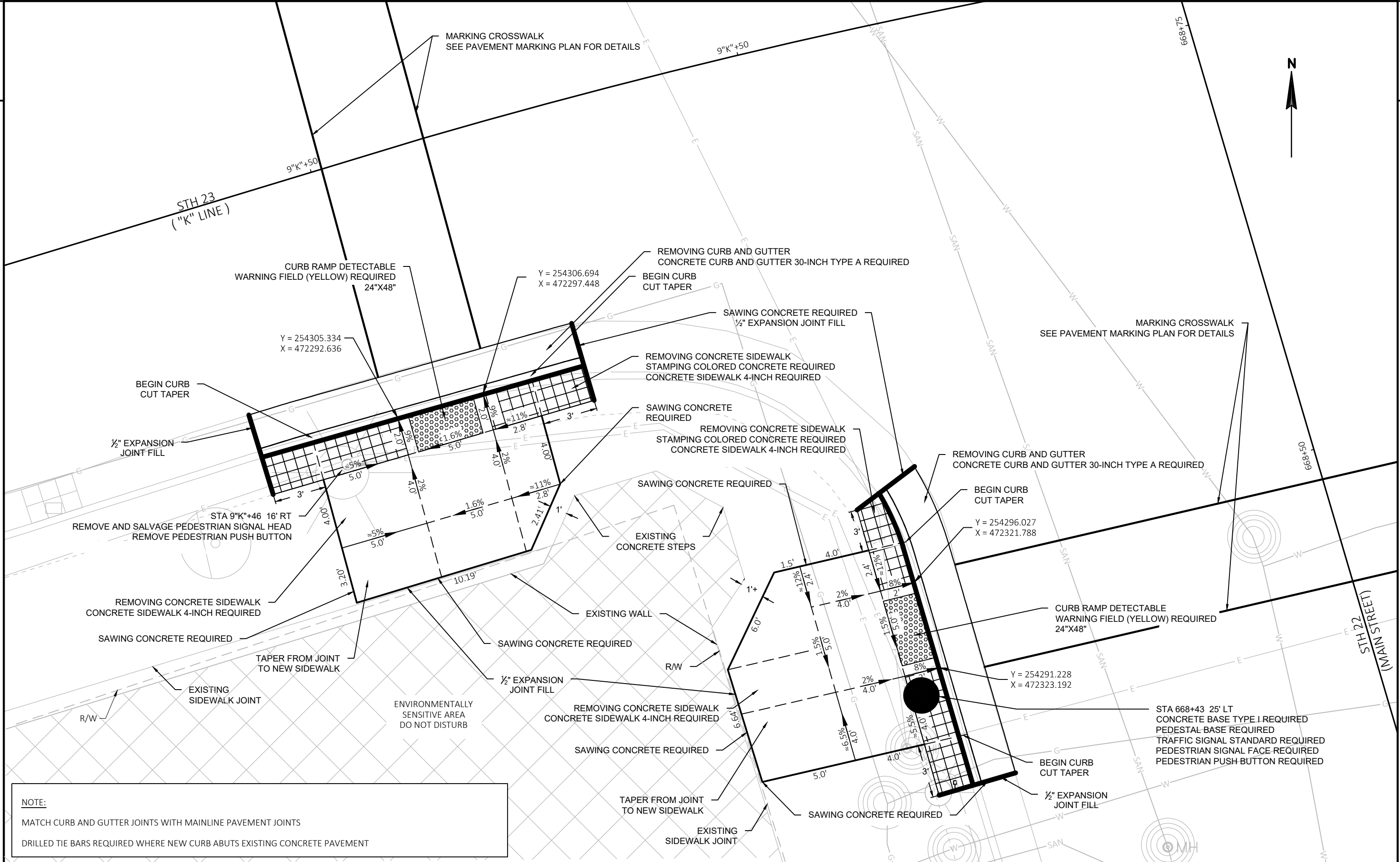
STH 22
(MAIN STREET)

647+25

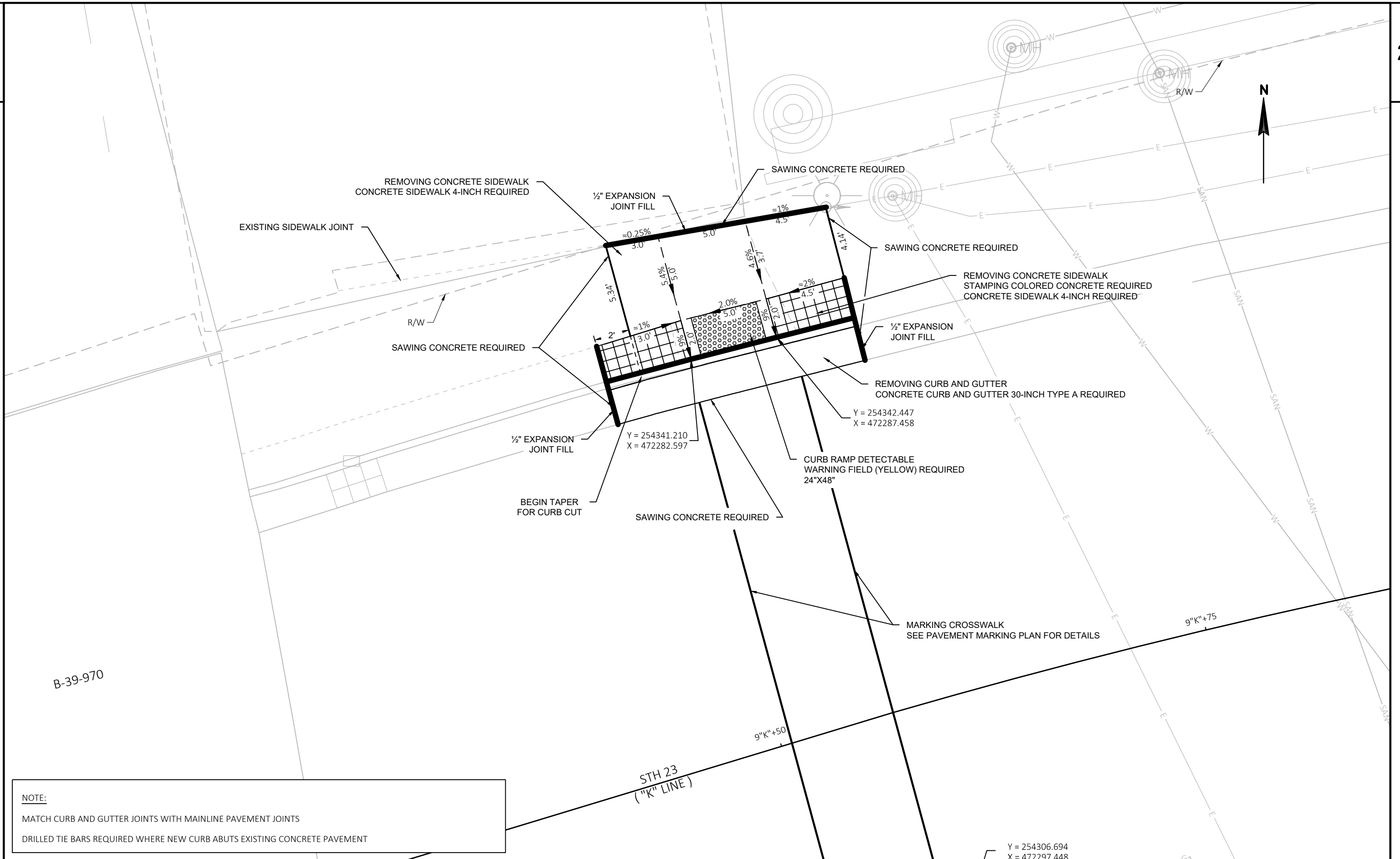
NOTE:
REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.



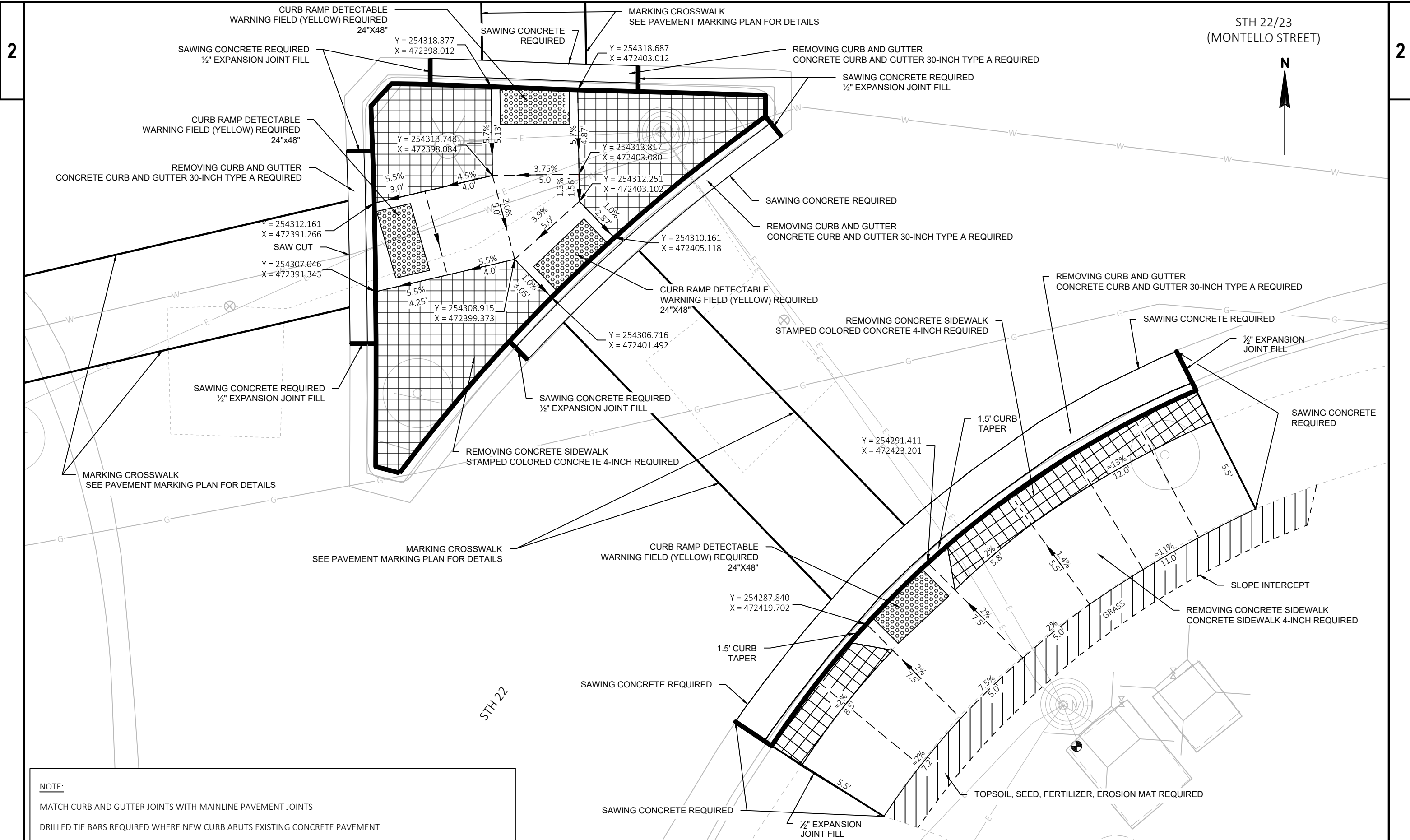
NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING ASPHALT PAVEMENT TO ALLOW FOR CURB FORMING.
 ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.



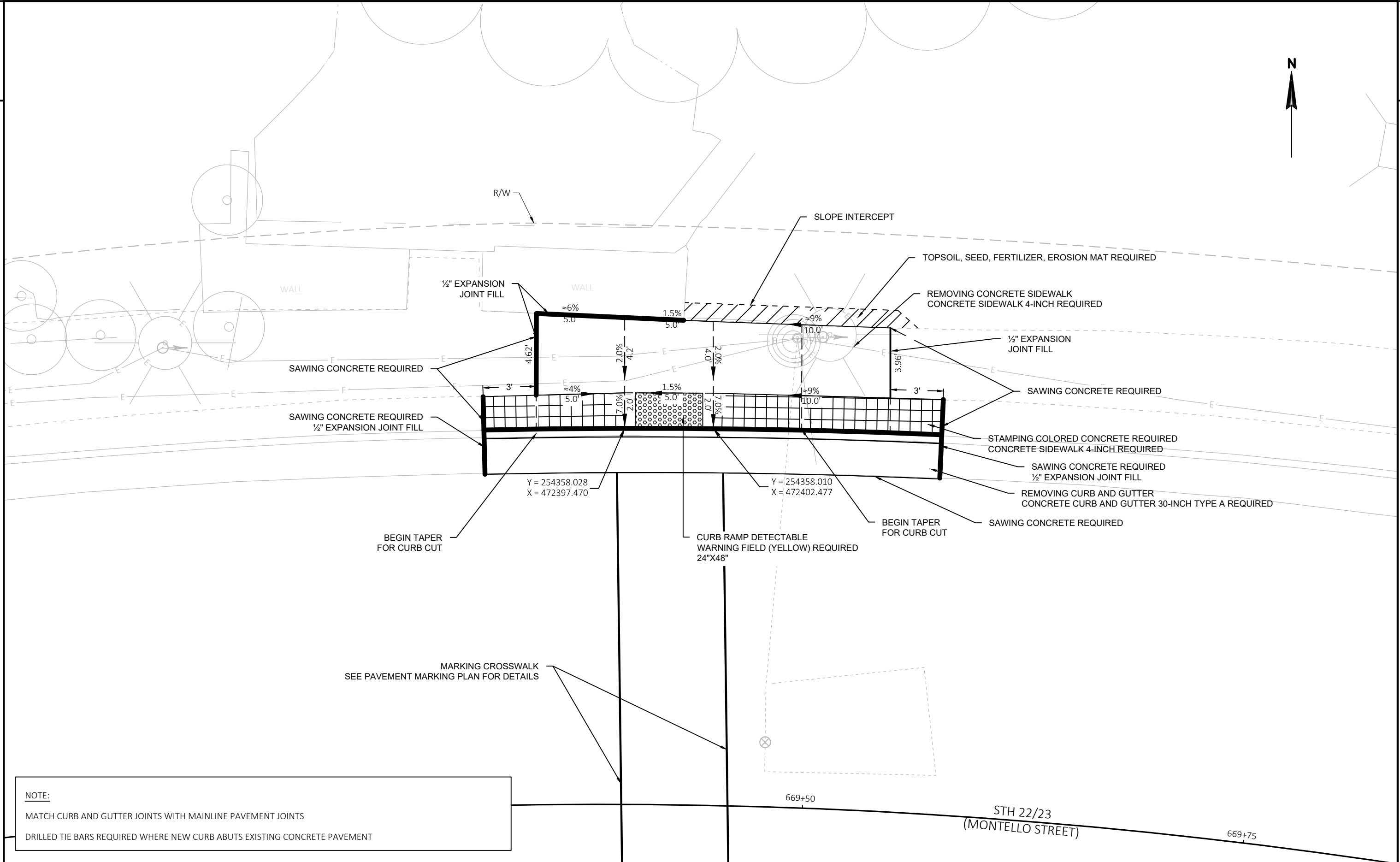
NOTE:
 MATCH CURB AND GUTTER JOINTS WITH MAINLINE PAVEMENT JOINTS
 DRILLED TIE BARS REQUIRED WHERE NEW CURB ABUTS EXISTING CONCRETE PAVEMENT



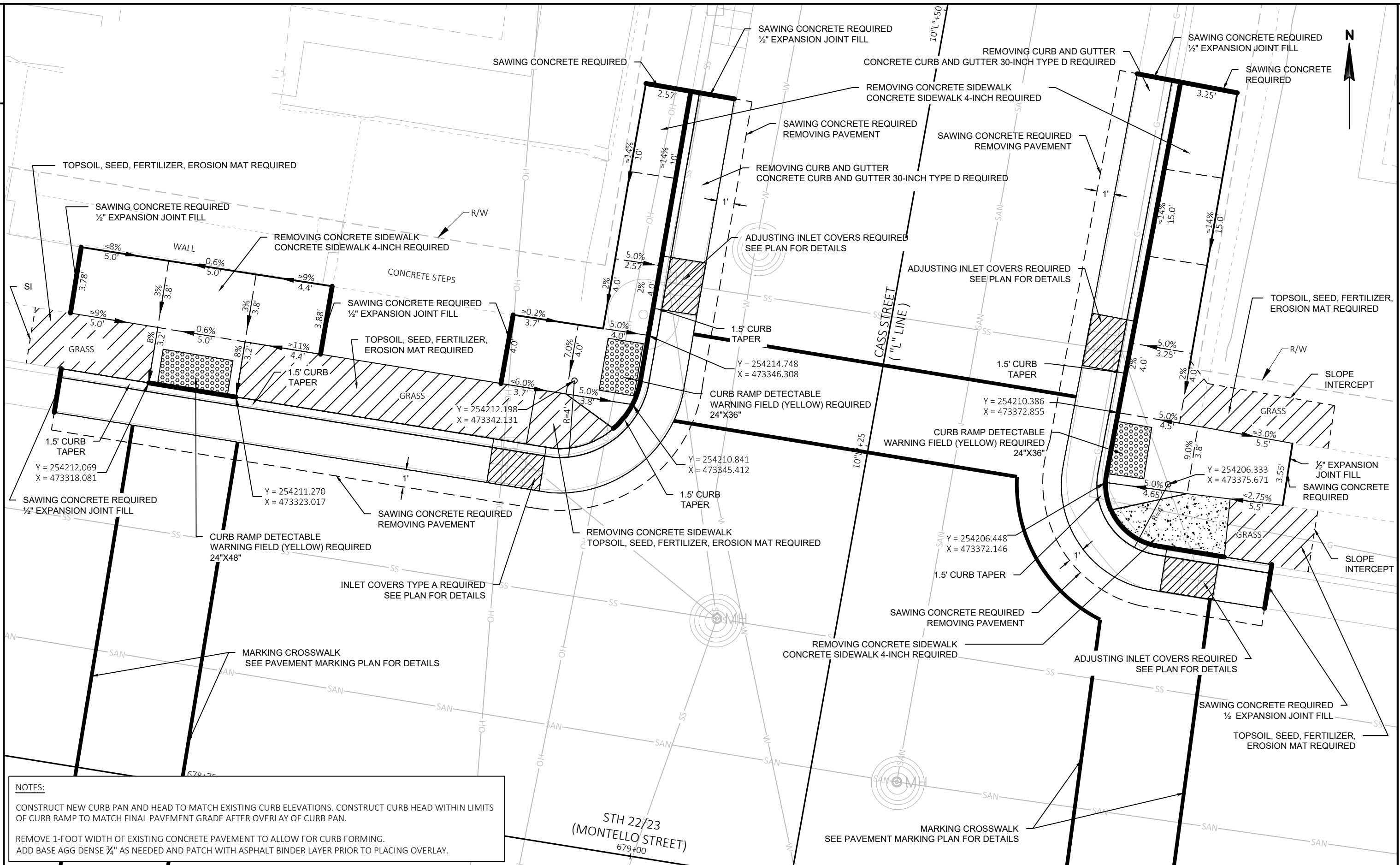
NOTE:
 MATCH CURB AND GUTTER JOINTS WITH MAINLINE PAVEMENT JOINTS
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NOTE:
 MATCH CURB AND GUTTER JOINTS WITH MAINLINE PAVEMENT JOINTS
 DRILLED TIE BARS REQUIRED WHERE NEW CURB ABUTS EXISTING CONCRETE PAVEMENT



NOTES:
 CONSTRUCT NEW CURB PAN AND HEAD TO MATCH EXISTING CURB ELEVATIONS. CONSTRUCT CURB HEAD WITHIN LIMITS OF CURB RAMP TO MATCH FINAL PAVEMENT GRADE AFTER OVERLAY OF CURB PAN.
 REMOVE 1-FOOT WIDTH OF EXISTING CONCRETE PAVEMENT TO ALLOW FOR CURB FORMING.
 ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

STH 22/23
(MONTELLO STREET)

CASS STREET
("L" LINE)

STA 679+09
STA 10"L"+00



MARKING CROSSWALK
SEE PAVEMENT MARKING PLAN FOR DETAILS

MARKING CROSSWALK
SEE PAVEMENT MARKING PLAN FOR DETAILS

TOPSOIL, SEEDING, FERTILIZER,
EROSION MAT REQUIRED

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILLER

Y = 254167.376
X = 473310.746

CURB RAMP DETECTABLE
WARNING FIELD (YELLOW) REQUIRED
24"X48"

SAWING CONCRETE REQUIRED
REMOVING PAVEMENT

Y = 254166.578
X = 473315.682

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

REMOVING CURB AND GUTTER
CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED

CURB RAMP DETECTABLE
WARNING FIELD (YELLOW) REQUIRED
24"X48"

Y = 254157.489
X = 473371.927

SAWING CONCRETE REQUIRED
REMOVING PAVEMENT

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILLER

REMOVING CURB AND GUTTER
CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED

1' CURB
TAPER

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILLER

TOPSOIL, SEEDING, FERTILIZER, EROSION MAT REQUIRED

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

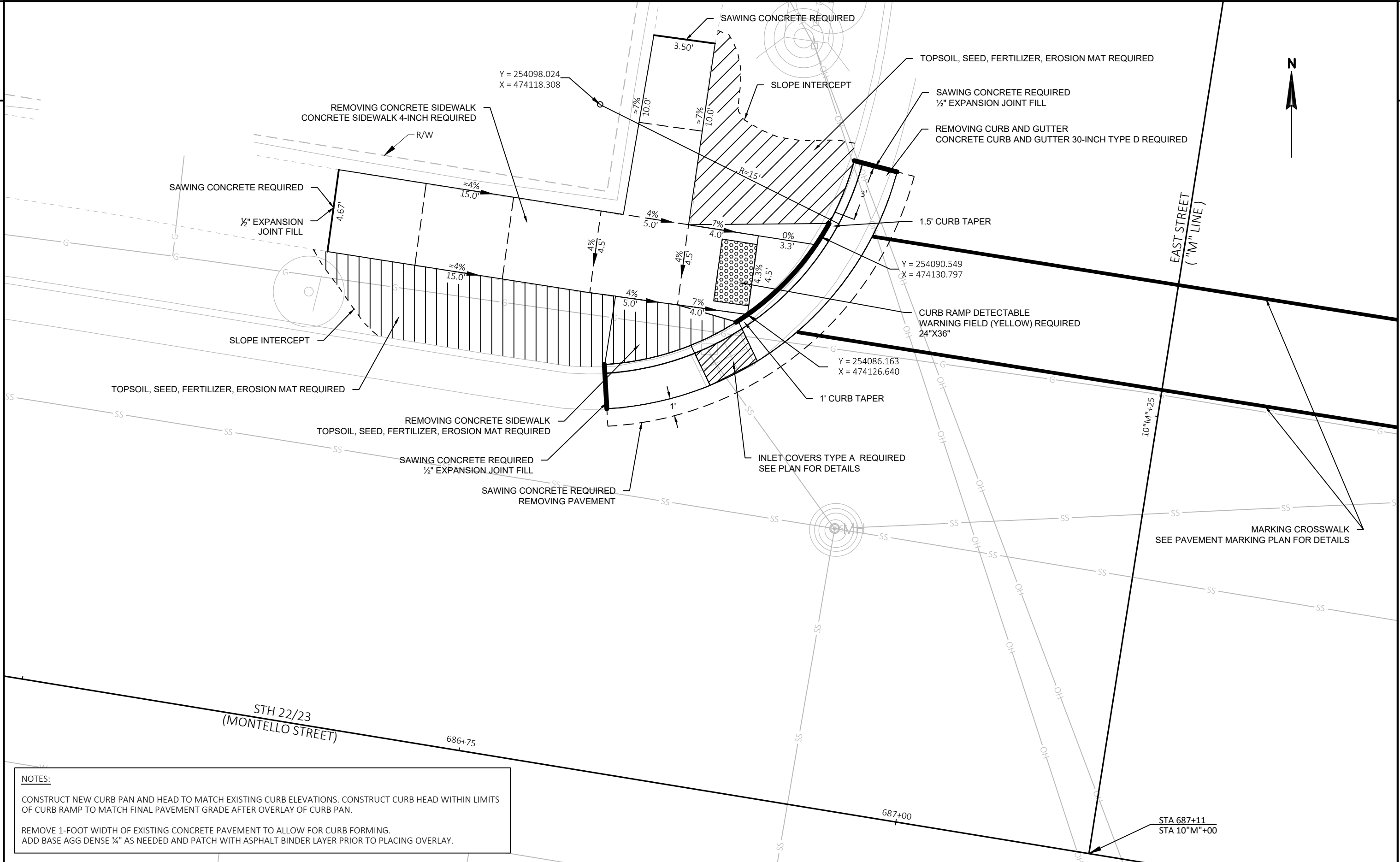
REMOVING CONCRETE SIDEWALK
CONCRETE SIDEWALK 4-INCH REQUIRED

SAWING CONCRETE REQUIRED
1/2" EXPANSION JOINT FILL

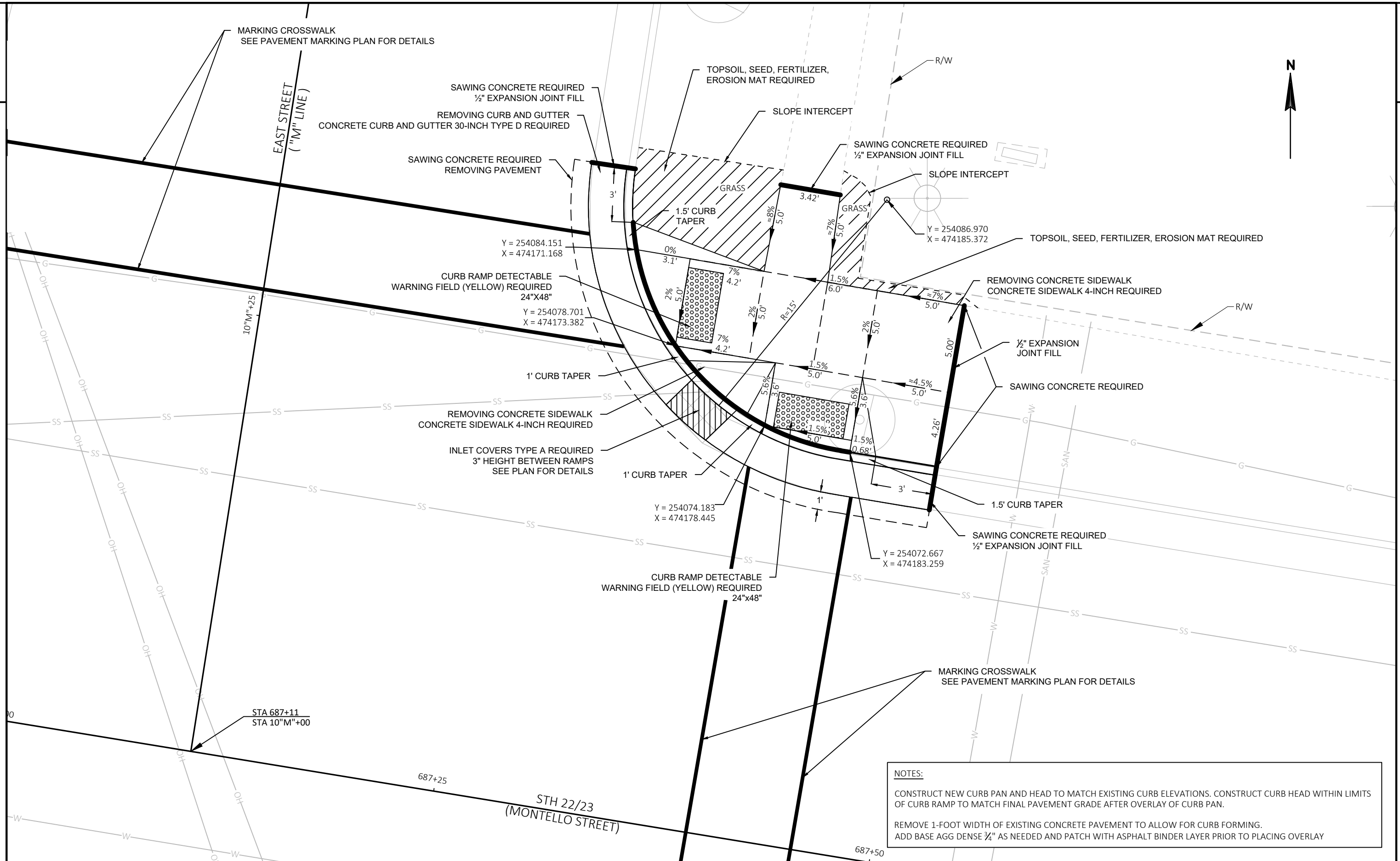
NOTES:

CONSTRUCT NEW CURB PAN AND HEAD TO MATCH EXISTING CURB ELEVATIONS. CONSTRUCT CURB HEAD WITHIN LIMITS OF CURB RAMP TO MATCH FINAL PAVEMENT GRADE AFTER OVERLAY OF CURB PAN.

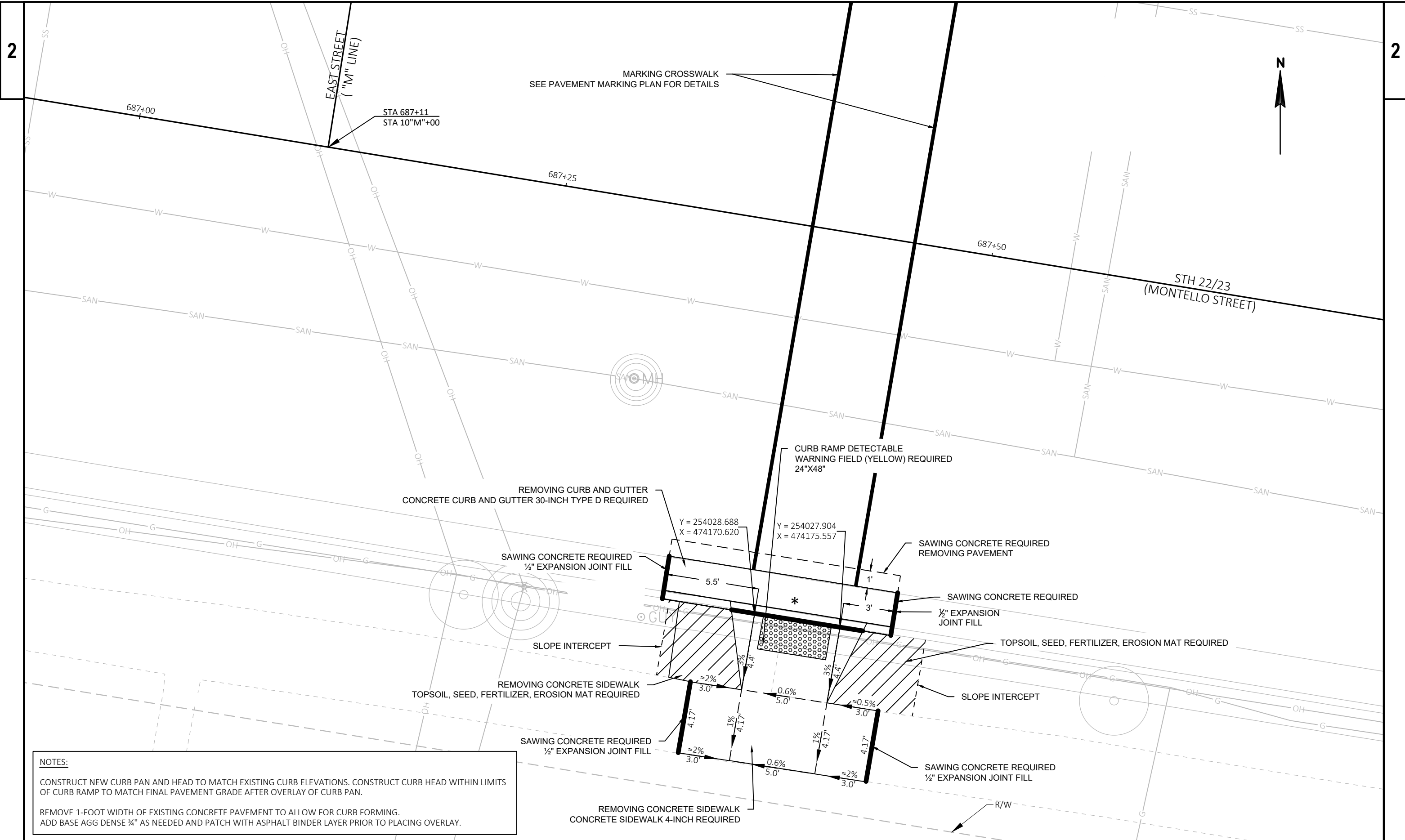
REMOVE 1-FOOT WIDTH OF EXISTING CONCRETE PAVEMENT TO ALLOW FOR CURB FORMING. ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.



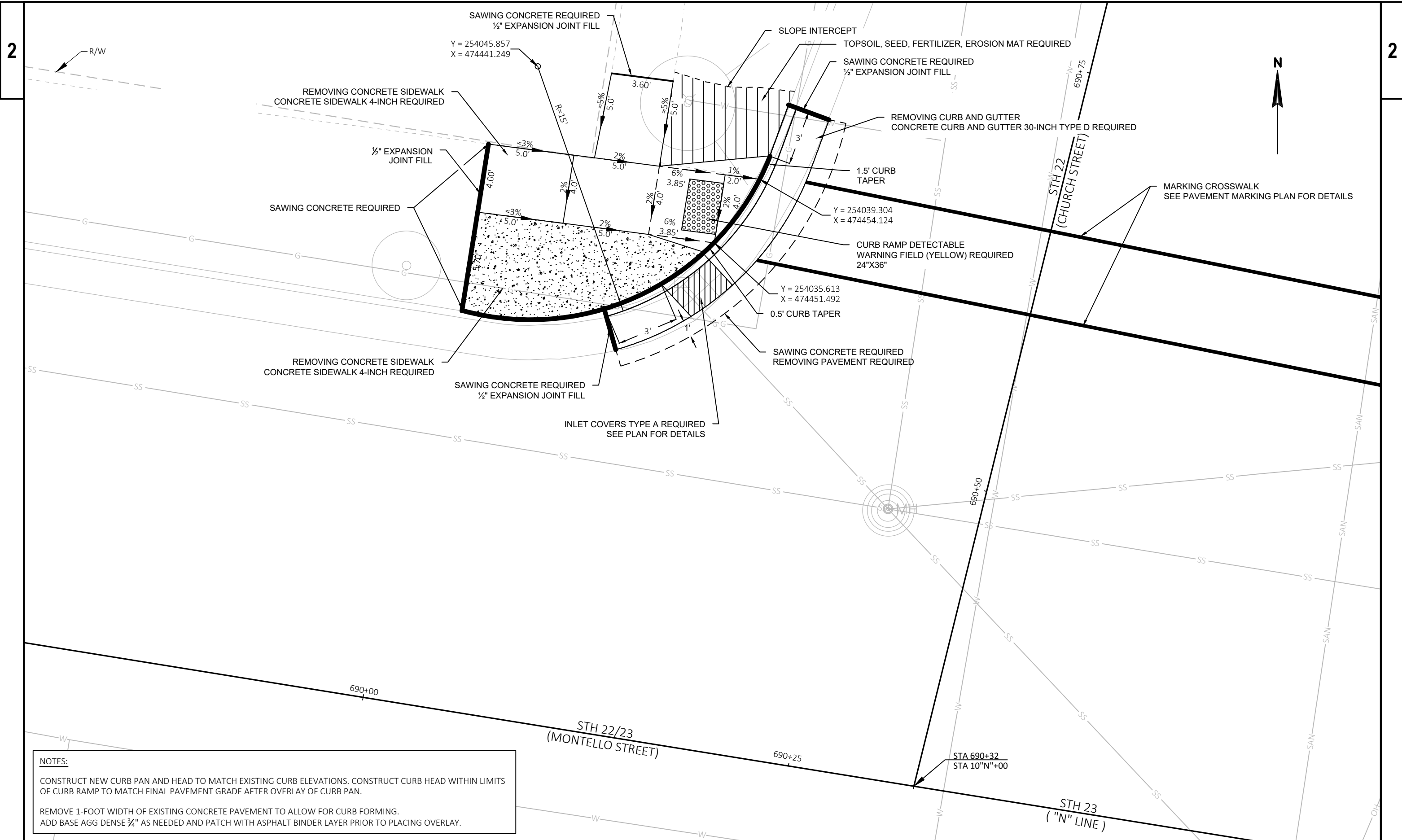
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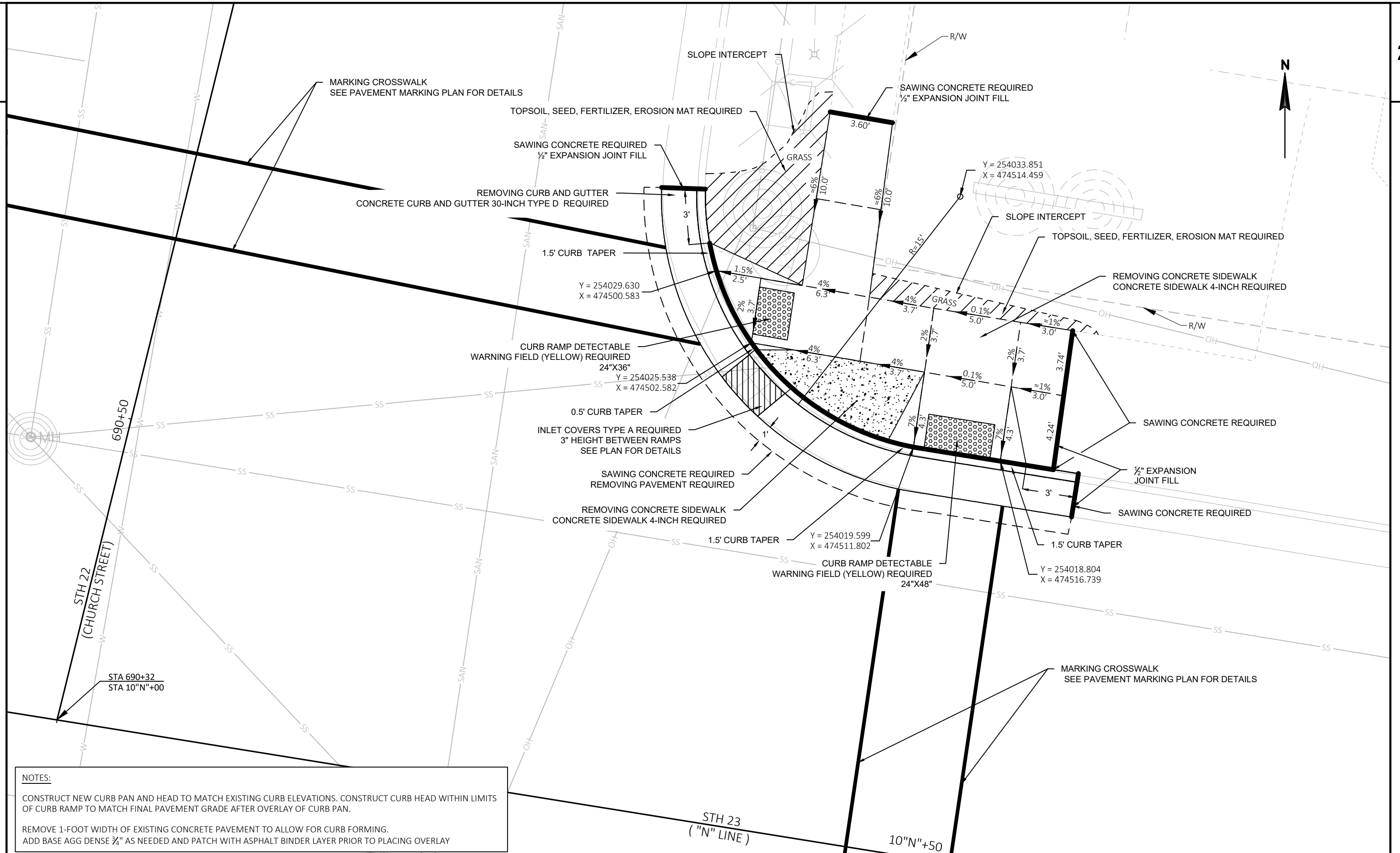
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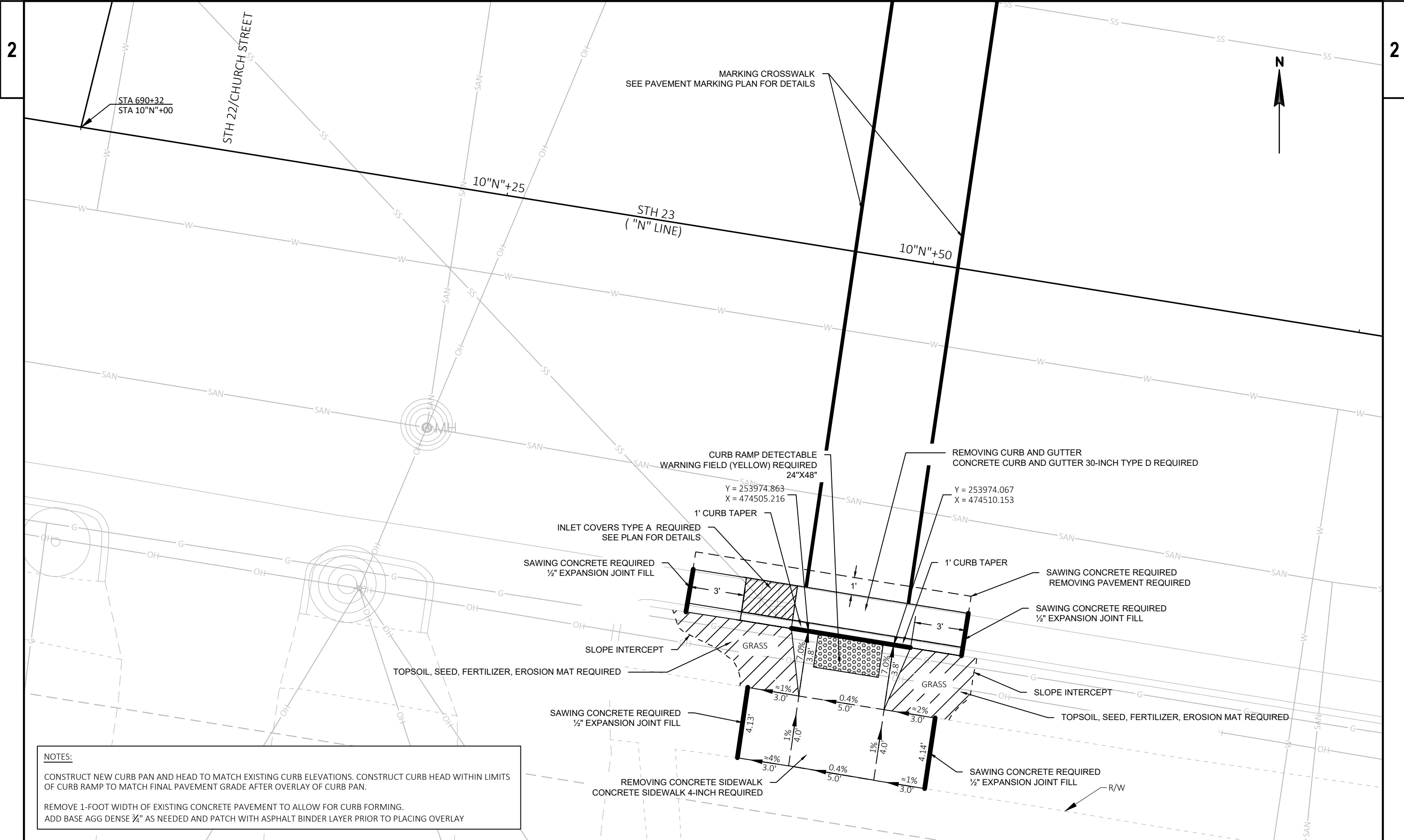
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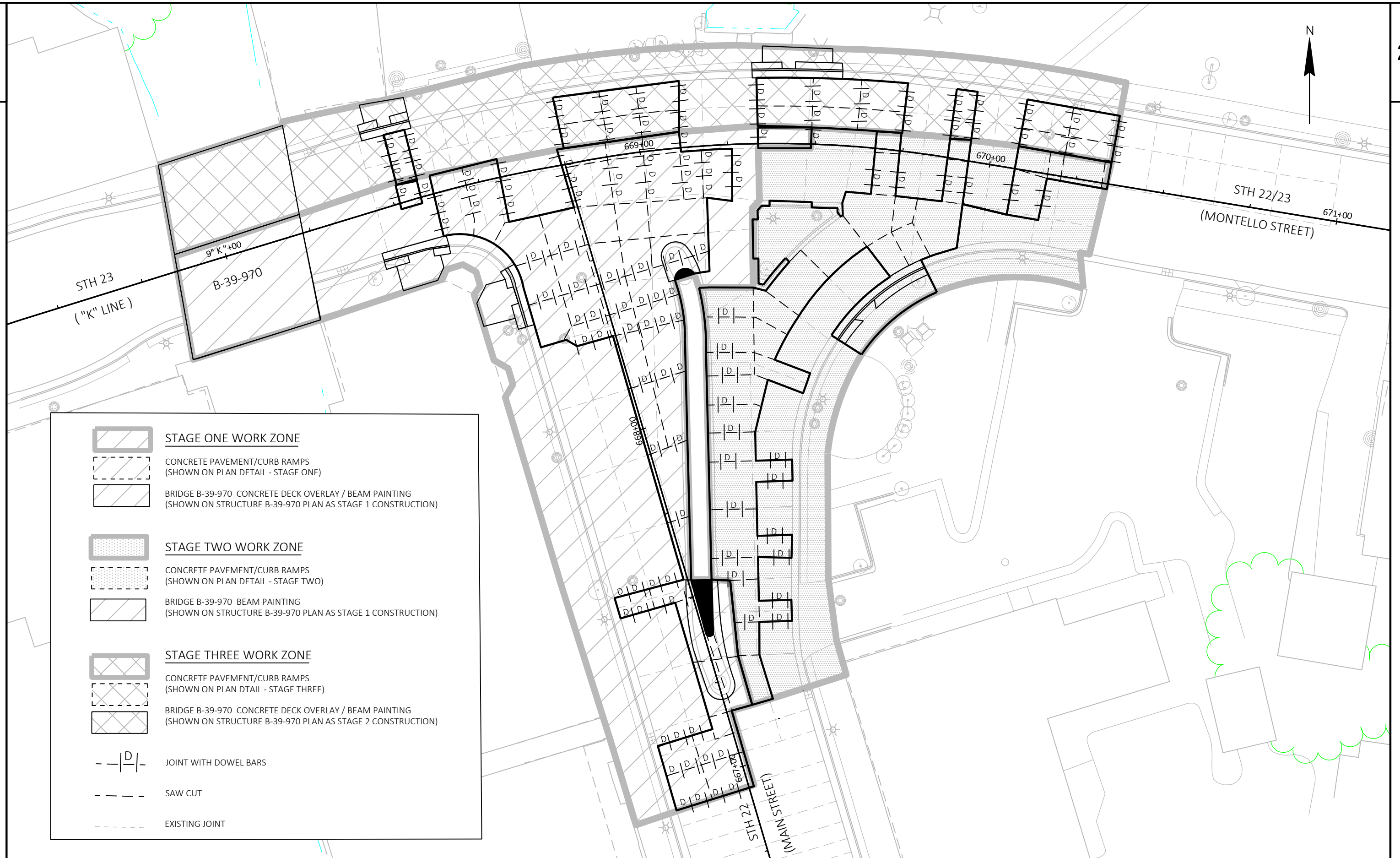
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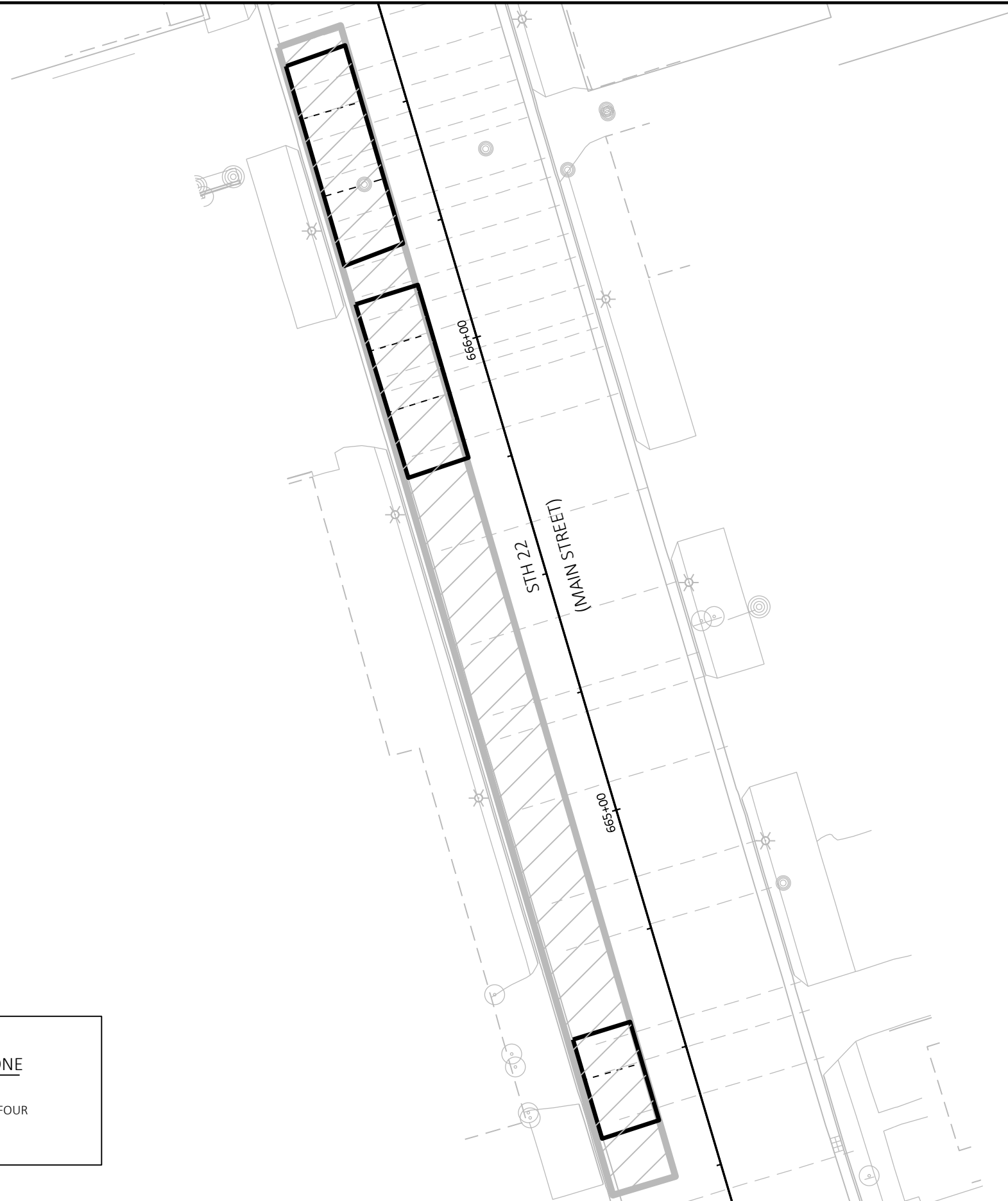
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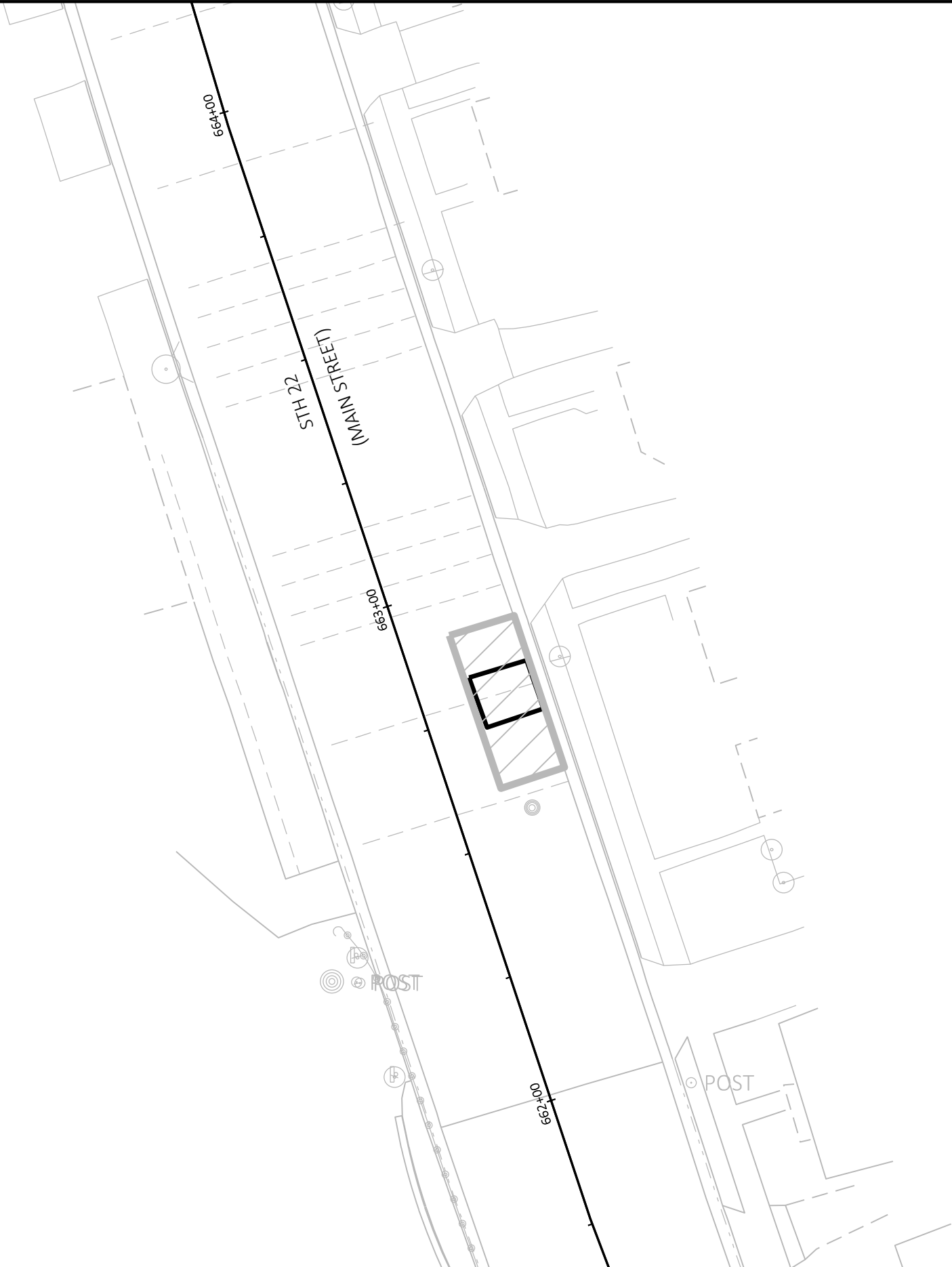




	STAGE ONE WORK ZONE
	CONCRETE PAVEMENT/CURB RAMPS (SHOWN ON PLAN DETAIL - STAGE ONE)
	BRIDGE B-39-970 CONCRETE DECK OVERLAY / BEAM PAINTING (SHOWN ON STRUCTURE B-39-970 PLAN AS STAGE 1 CONSTRUCTION)
	STAGE TWO WORK ZONE
	CONCRETE PAVEMENT/CURB RAMPS (SHOWN ON PLAN DETAIL - STAGE TWO)
	BRIDGE B-39-970 BEAM PAINTING (SHOWN ON STRUCTURE B-39-970 PLAN AS STAGE 1 CONSTRUCTION)
	STAGE THREE WORK ZONE
	CONCRETE PAVEMENT/CURB RAMPS (SHOWN ON PLAN DETAIL - STAGE THREE)
	BRIDGE B-39-970 CONCRETE DECK OVERLAY / BEAM PAINTING (SHOWN ON STRUCTURE B-39-970 PLAN AS STAGE 2 CONSTRUCTION)
	JOINT WITH DOWEL BARS
	SAW CUT
	EXISTING JOINT

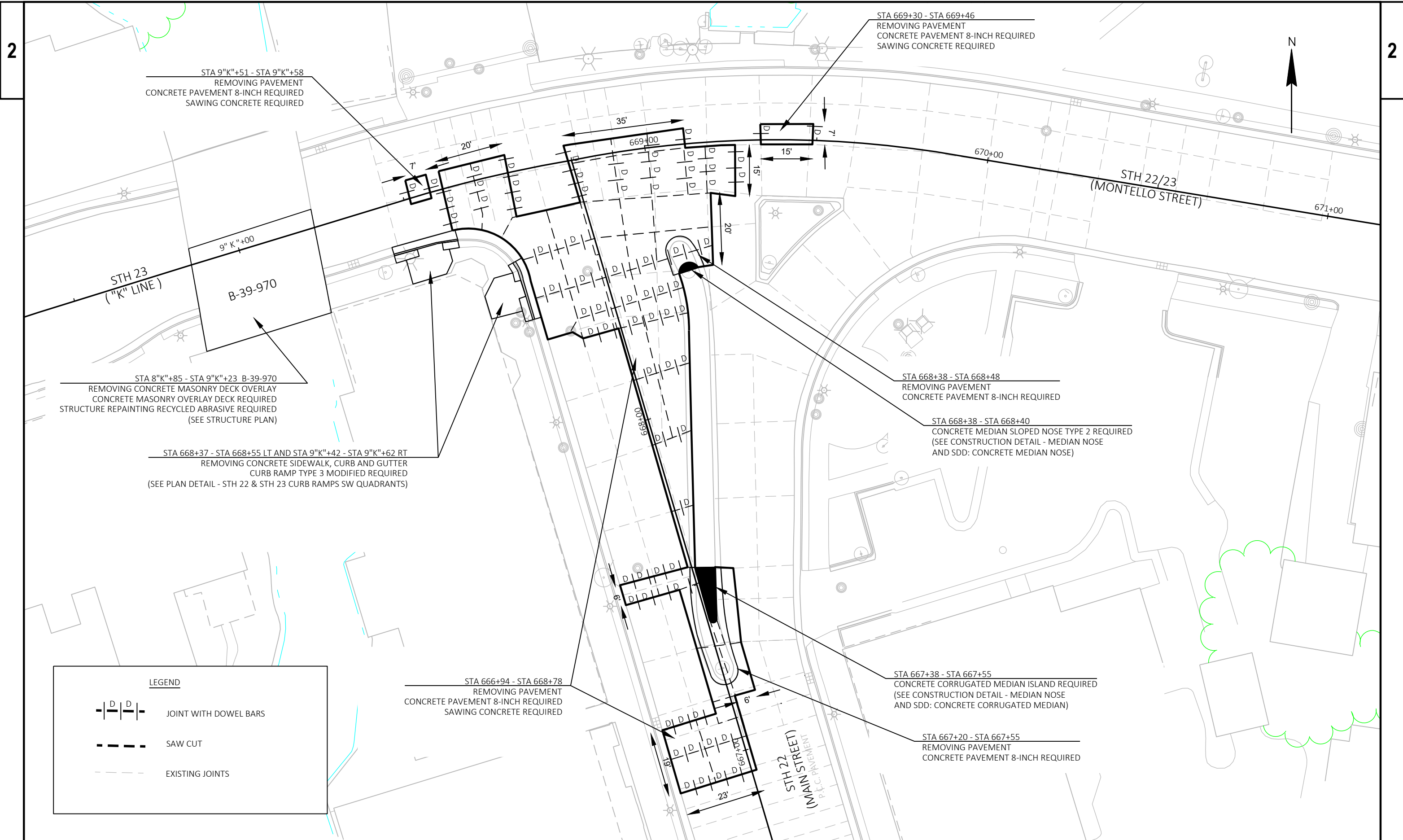


	STAGE FOUR WORK ZONE
	CONCRETE PAVEMENT - STAGE FOUR





	STAGE FIVE WORK ZONE
	CONCRETE PAVEMENT - STAGE FIVE



STA 9"K"+51 - STA 9"K"+58
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STA 669+30 - STA 669+46
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STA 8"K"+85 - STA 9"K"+23 B-39-970
 REMOVING CONCRETE MASONRY DECK OVERLAY
 CONCRETE MASONRY OVERLAY DECK REQUIRED
 STRUCTURE REPAINTING RECYCLED ABRASIVE REQUIRED
 (SEE STRUCTURE PLAN)

STA 668+38 - STA 668+48
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED

STA 668+37 - STA 668+55 LT AND STA 9"K"+42 - STA 9"K"+62 RT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 3 MODIFIED REQUIRED
 (SEE PLAN DETAIL - STH 22 & STH 23 CURB RAMPS SW QUADRANTS)

STA 668+38 - STA 668+40
 CONCRETE MEDIAN SLOPED NOSE TYPE 2 REQUIRED
 (SEE CONSTRUCTION DETAIL - MEDIAN NOSE
 AND SDD: CONCRETE MEDIAN NOSE)

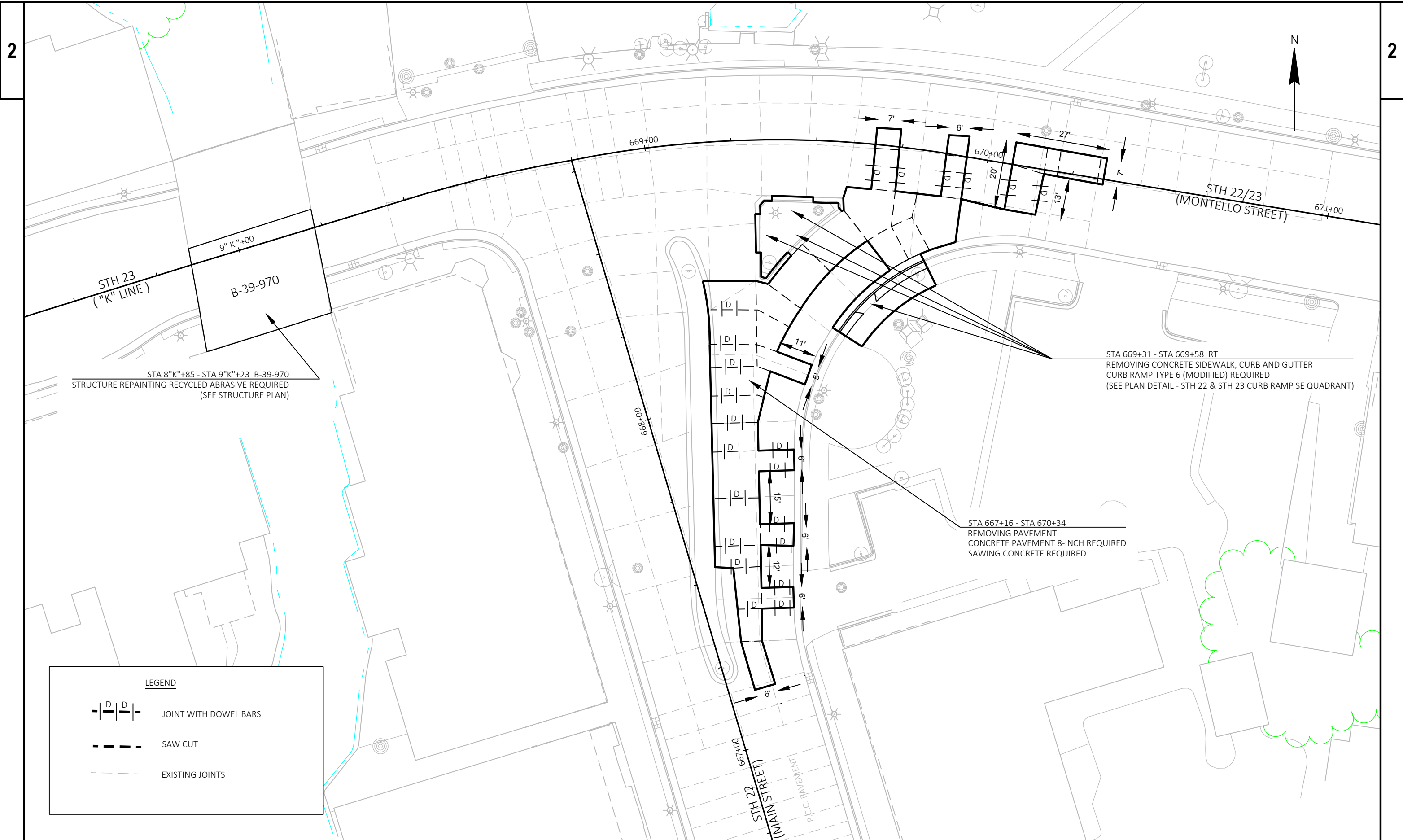
STA 666+94 - STA 668+78
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STA 667+38 - STA 667+55
 CONCRETE CORRUGATED MEDIAN ISLAND REQUIRED
 (SEE CONSTRUCTION DETAIL - MEDIAN NOSE
 AND SDD: CONCRETE CORRUGATED MEDIAN)

STA 667+20 - STA 667+55
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED

LEGEND

- JOINT WITH DOWEL BARS
- SAW CUT
- EXISTING JOINTS



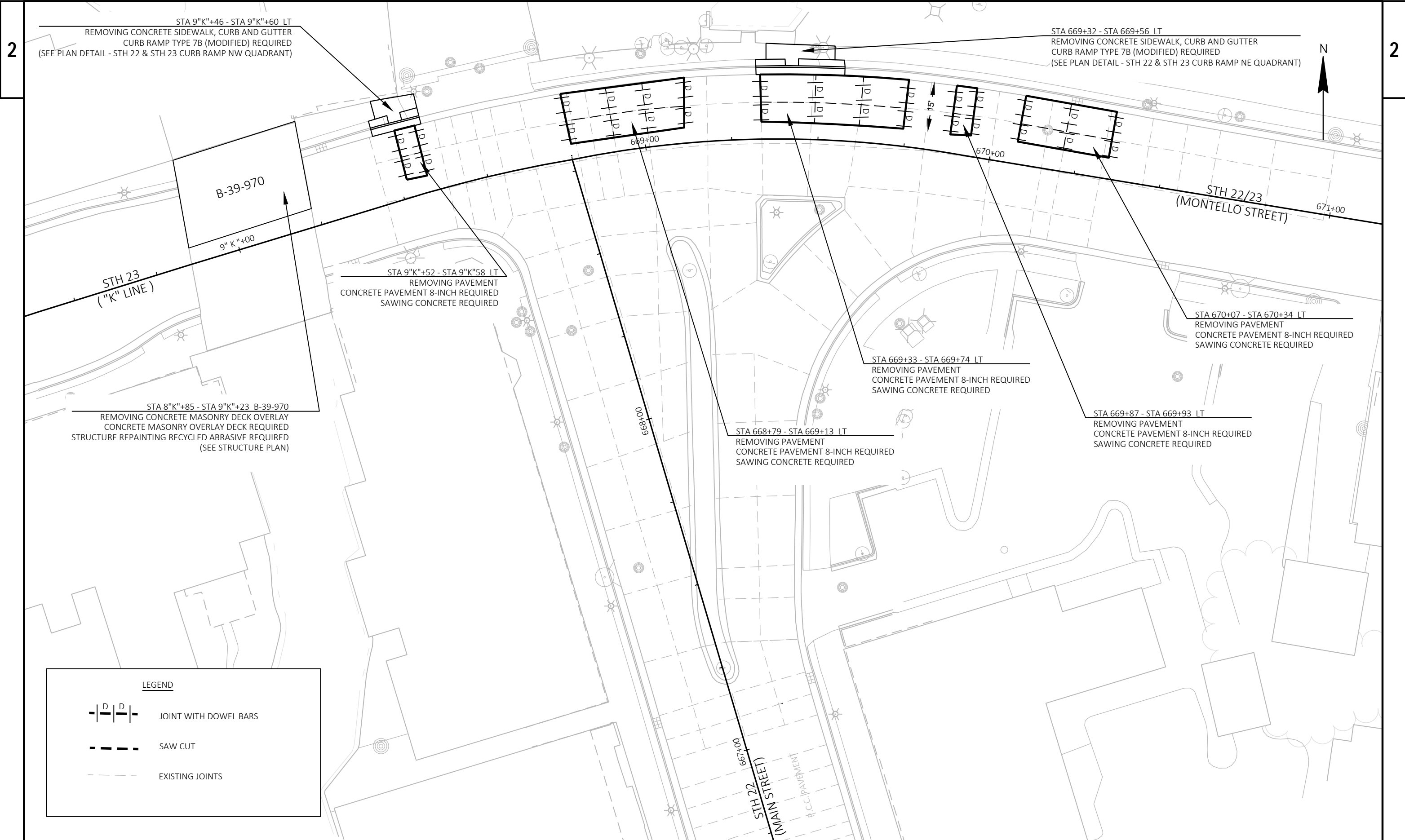
STA 8"K"+85 - STA 9"K"+23 B-39-970
 STRUCTURE REPAINTING RECYCLED ABRASIVE REQUIRED
 (SEE STRUCTURE PLAN)

STA 669+31 - STA 669+58 RT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 6 (MODIFIED) REQUIRED
 (SEE PLAN DETAIL - STH 22 & STH 23 CURB RAMP SE QUADRANT)

STA 667+16 - STA 670+34
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

LEGEND

- JOINT WITH DOWEL BARS
- SAW CUT
- EXISTING JOINTS



STA 9"K"+46 - STA 9"K"+60 LT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 7B (MODIFIED) REQUIRED
 (SEE PLAN DETAIL - STH 22 & STH 23 CURB RAMP NW QUADRANT)

STA 669+32 - STA 669+56 LT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 7B (MODIFIED) REQUIRED
 (SEE PLAN DETAIL - STH 22 & STH 23 CURB RAMP NE QUADRANT)

B-39-970

STH 23
 ("K" LINE)

STA 9"K"+52 - STA 9"K"+58 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STA 8"K"+85 - STA 9"K"+23 B-39-970
 REMOVING CONCRETE MASONRY DECK OVERLAY
 CONCRETE MASONRY OVERLAY DECK REQUIRED
 STRUCTURE REPAINTING RECYCLED ABRASIVE REQUIRED
 (SEE STRUCTURE PLAN)

STA 669+33 - STA 669+74 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STA 670+07 - STA 670+34 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STA 668+79 - STA 669+13 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STA 669+87 - STA 669+93 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

LEGEND

- JOINT WITH DOWEL BARS
- SAW CUT
- EXISTING JOINTS



STA 666+25 - STA 666+62 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED




STA 665+79 - STA 666+13 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

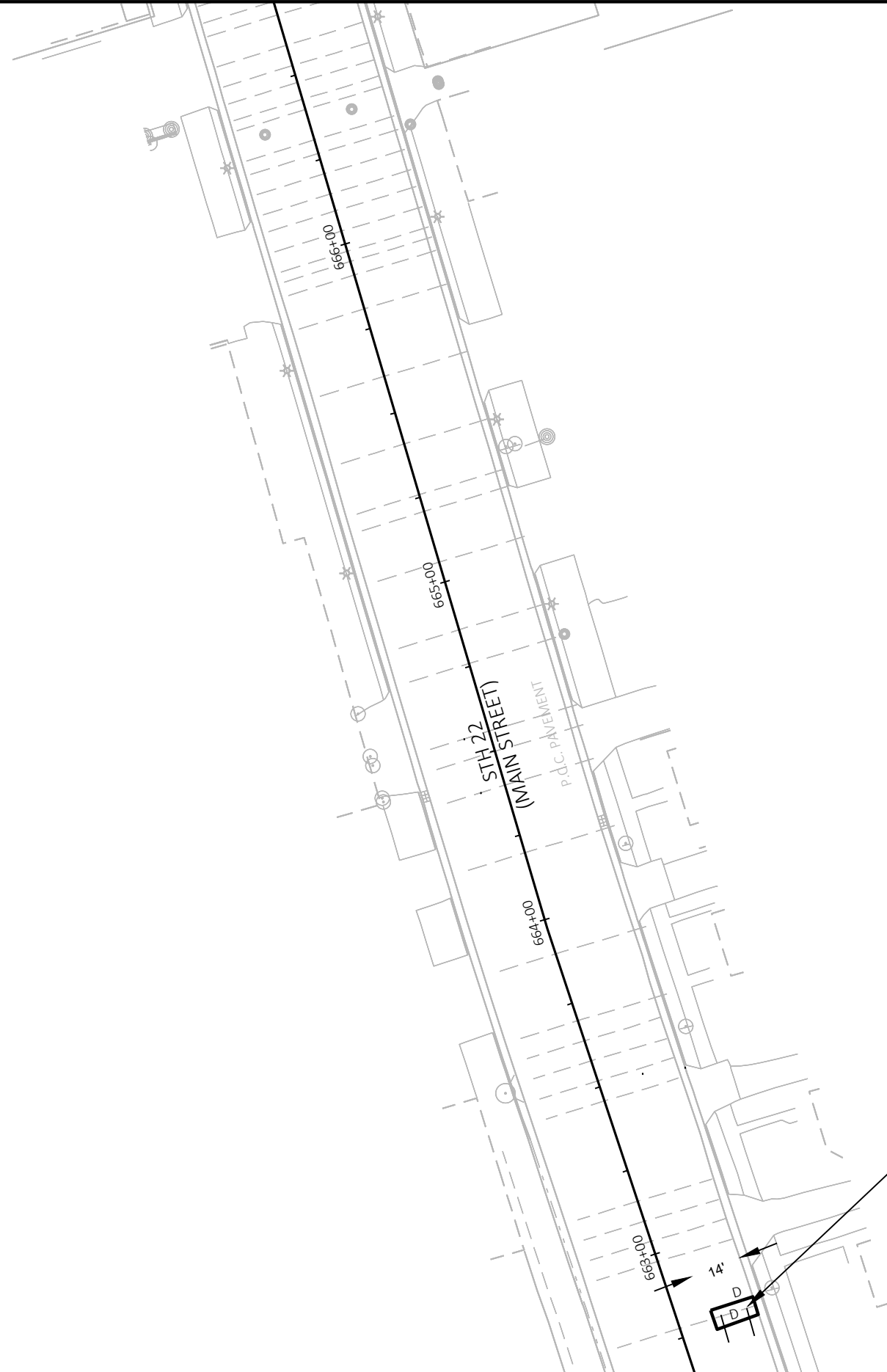
STA 664+40 - STA 664+55 LT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

STH 22
 (MAIN STREET)




14'

LEGEND

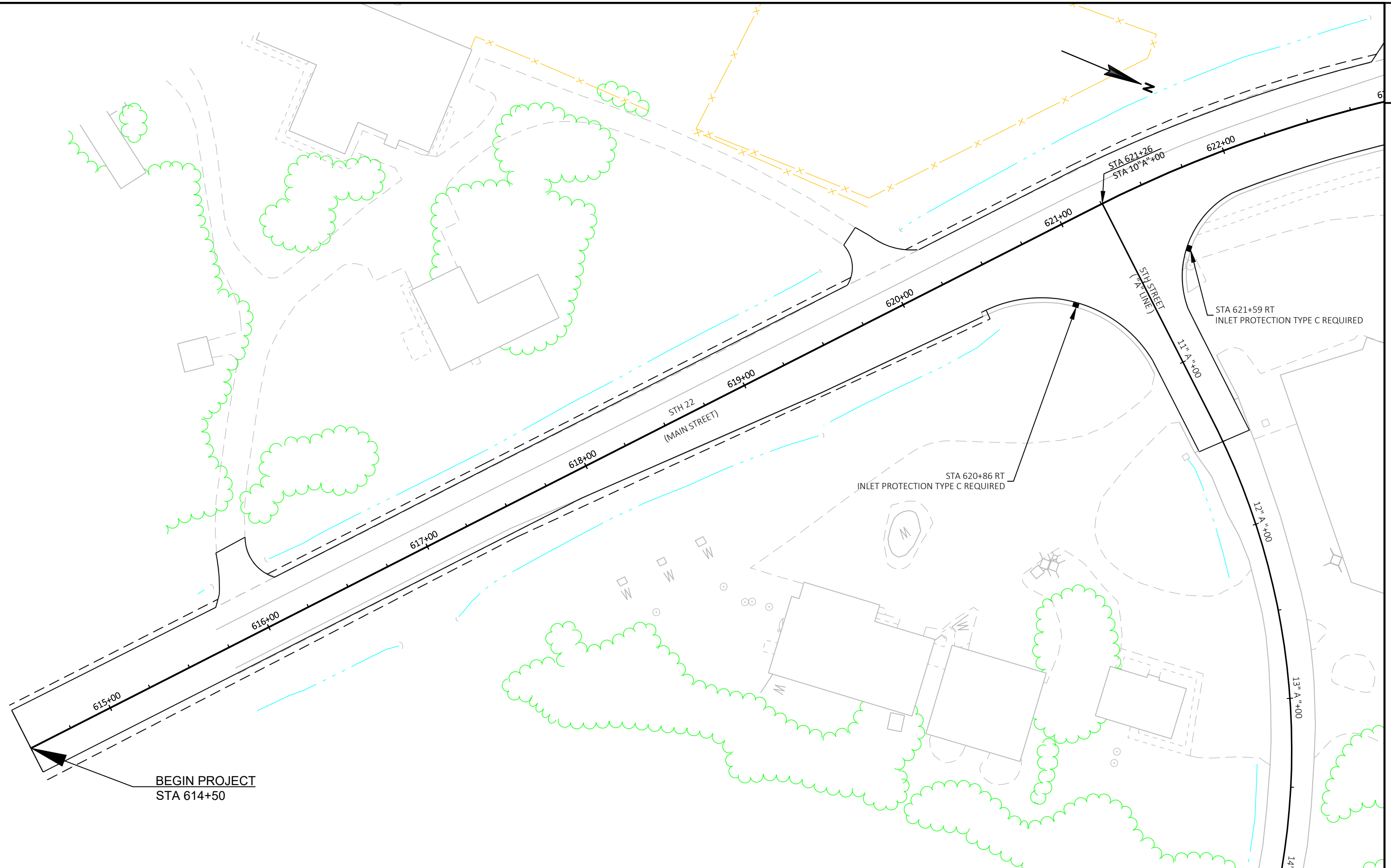
-  JOINT WITH DOWEL BARS
-  SAW CUT
-  EXISTING JOINTS

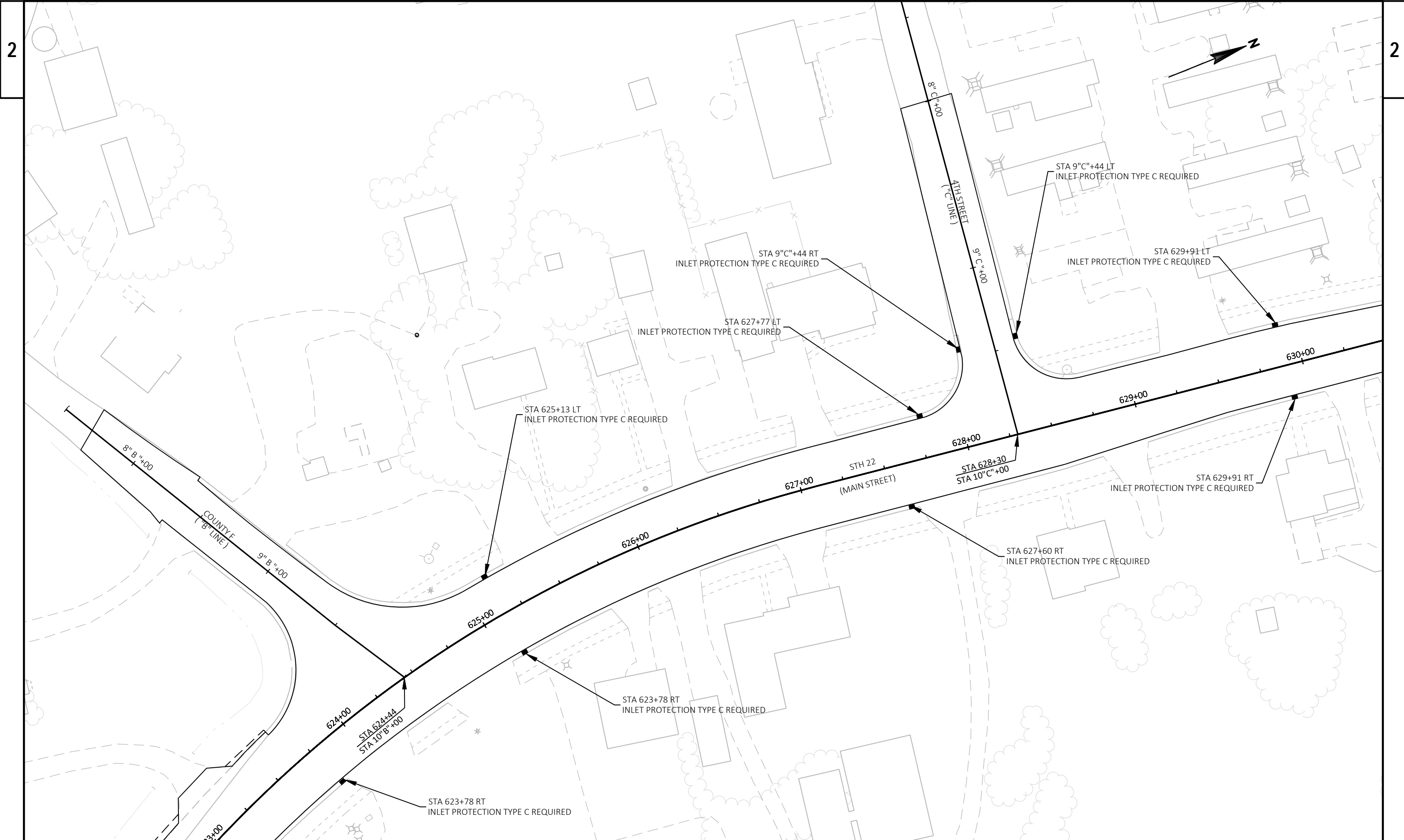


LEGEND

-  JOINT WITH DOWEL BARS
-  SAW CUT
-  EXISTING JOINTS

STA 662+75 - STA 662+81 RT
 REMOVING PAVEMENT
 CONCRETE PAVEMENT 8-INCH REQUIRED
 SAWING CONCRETE REQUIRED

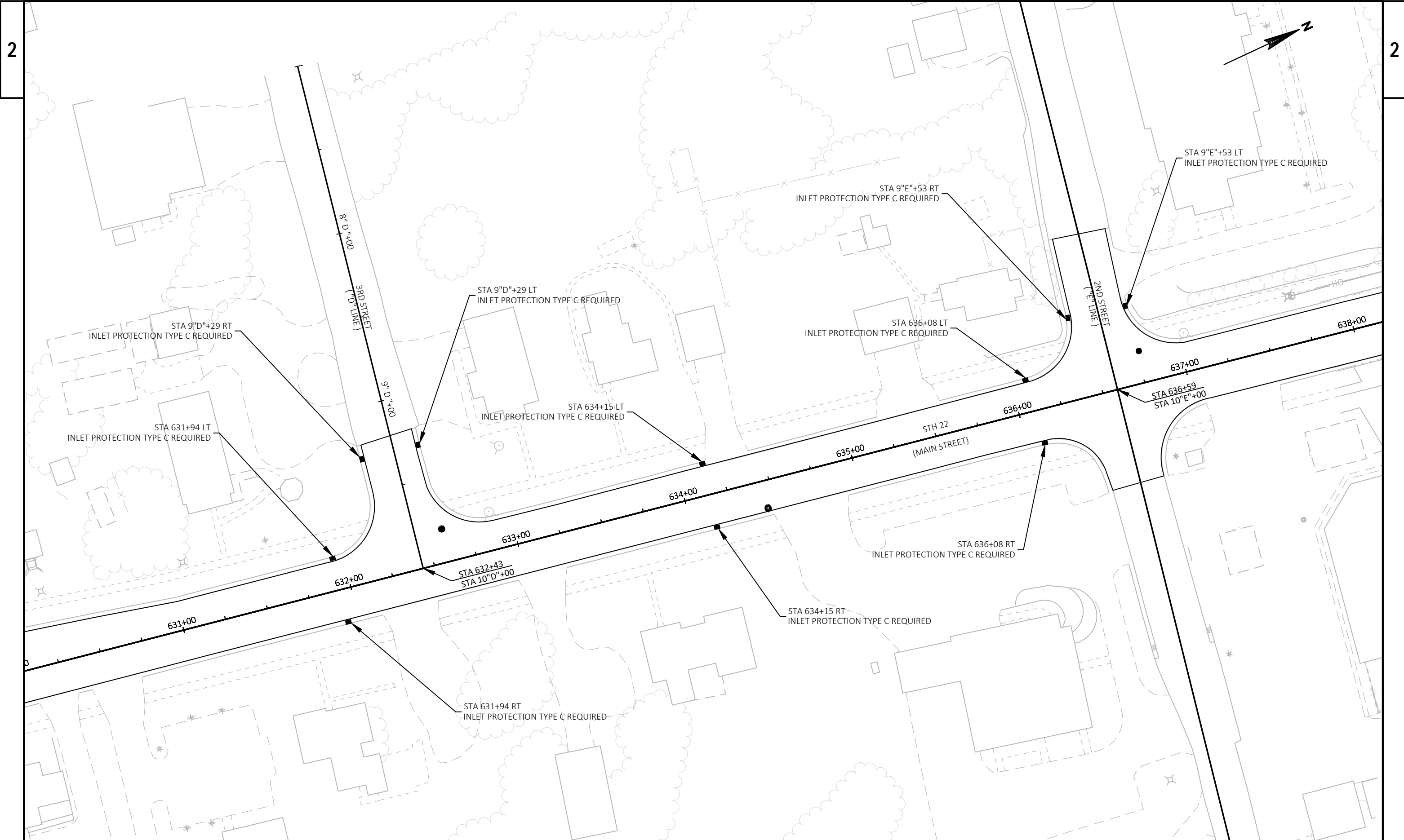




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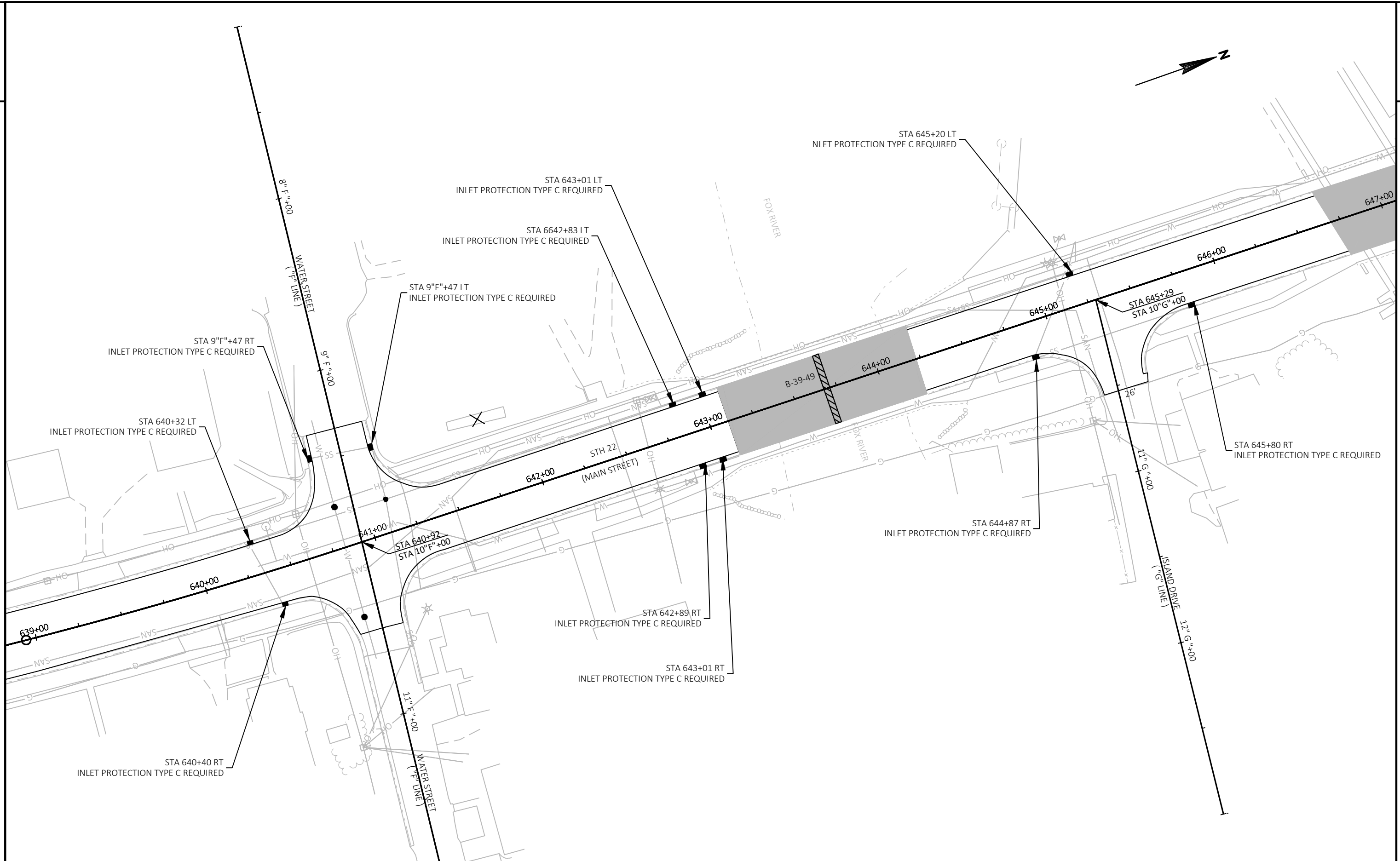
PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	EROSION CONTROL	SHEET	E
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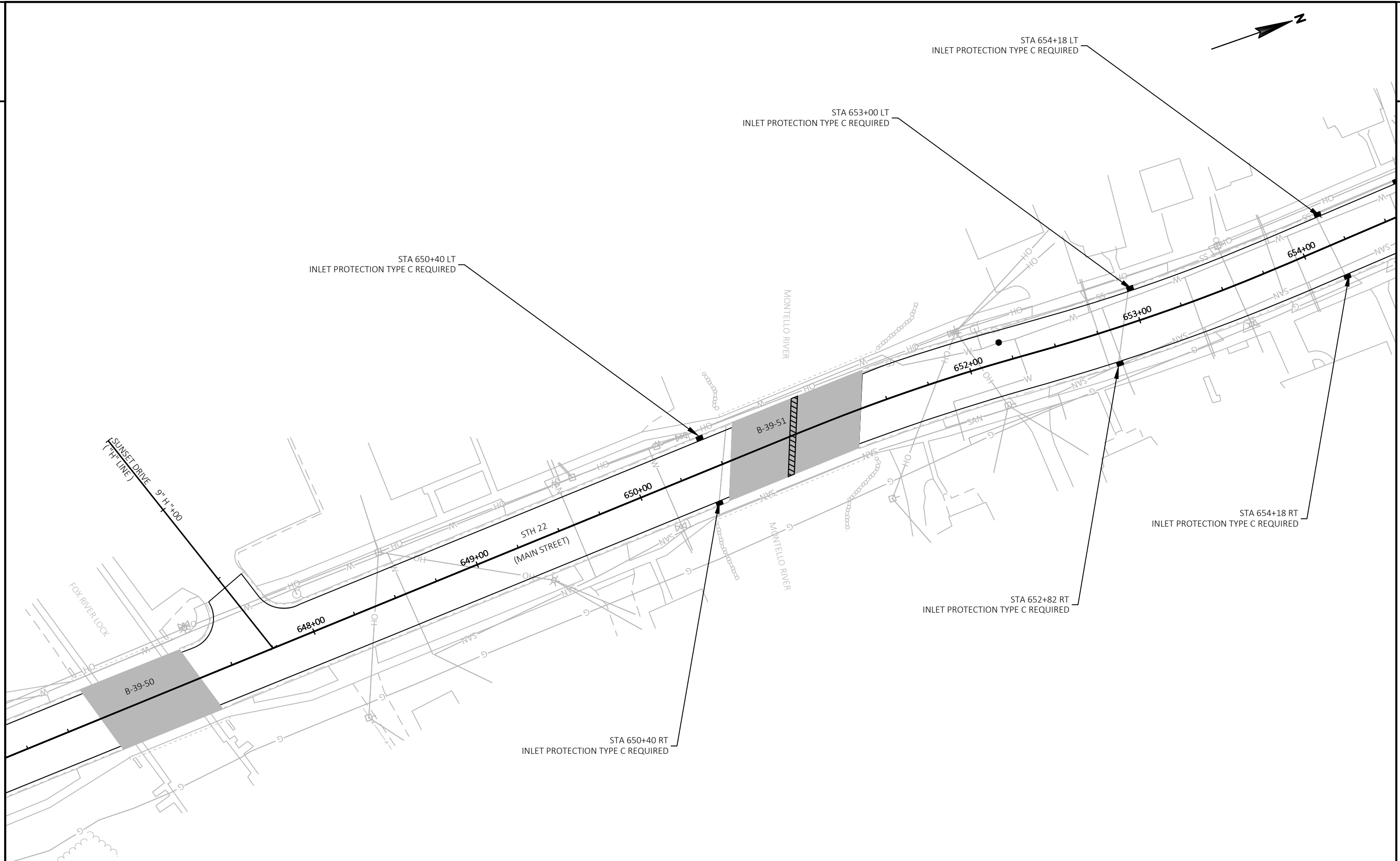
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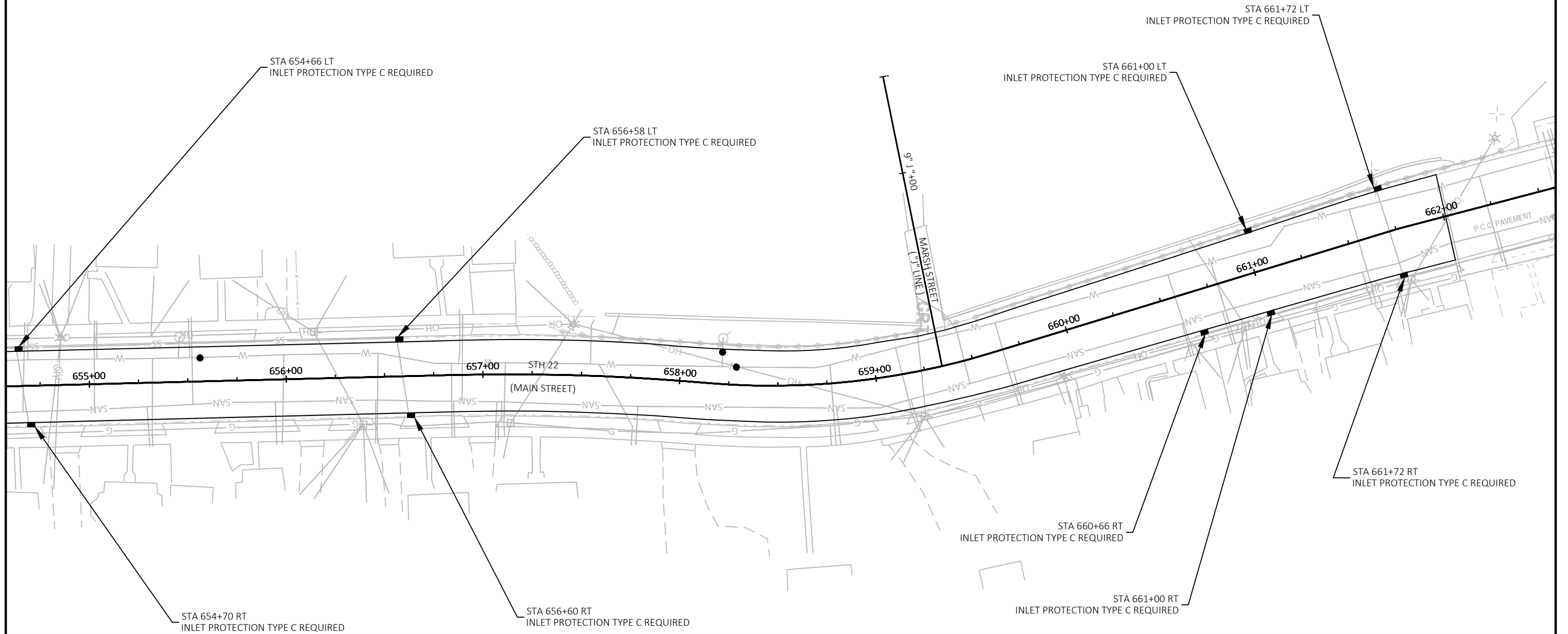
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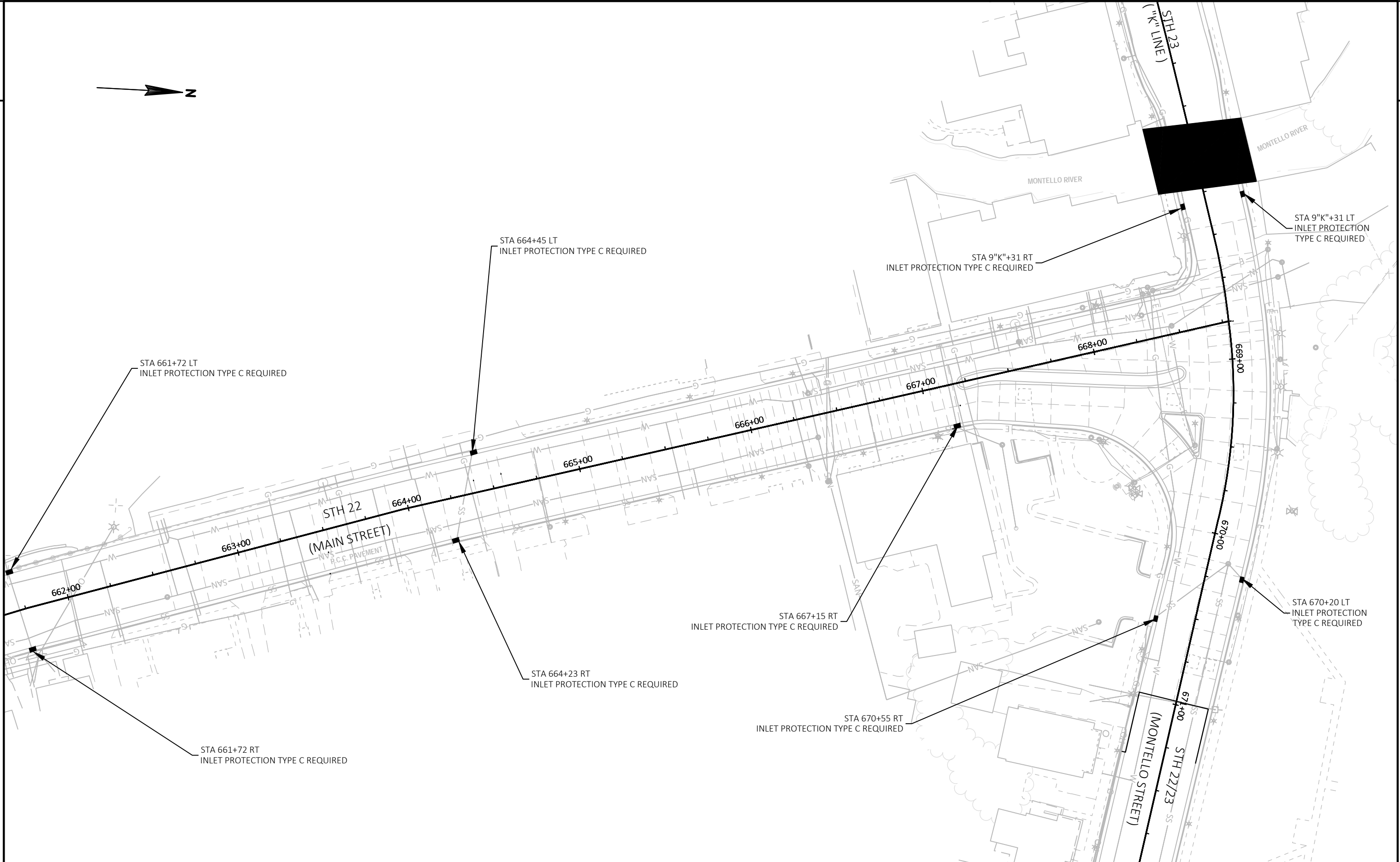
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PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	EROSION CONTROL	SHEET	E
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PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	EROSION CONTROL	SHEET	E
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PROJECT NO: 6054-05-75

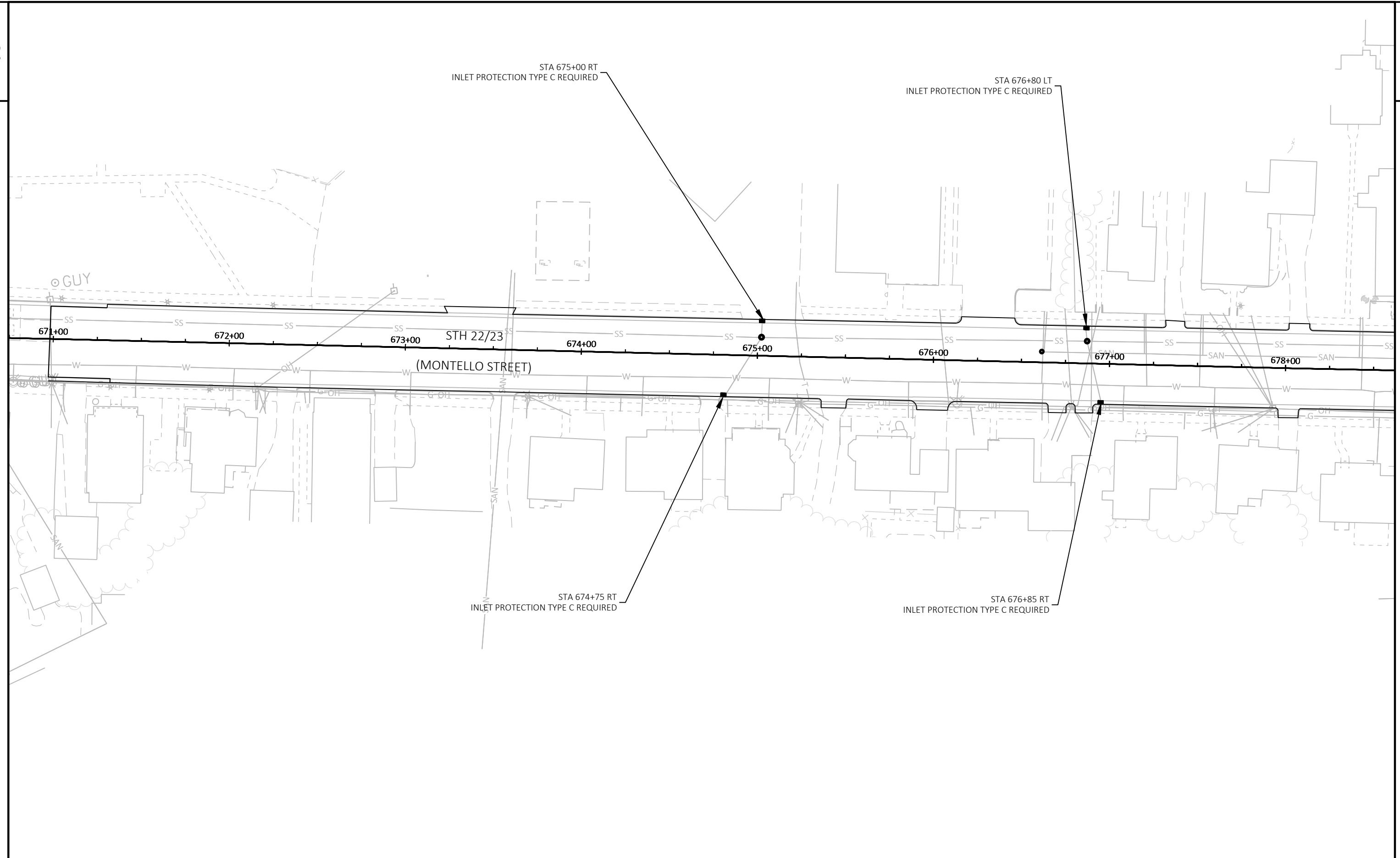
HWY: STH 22

COUNTY: MARQUETTE

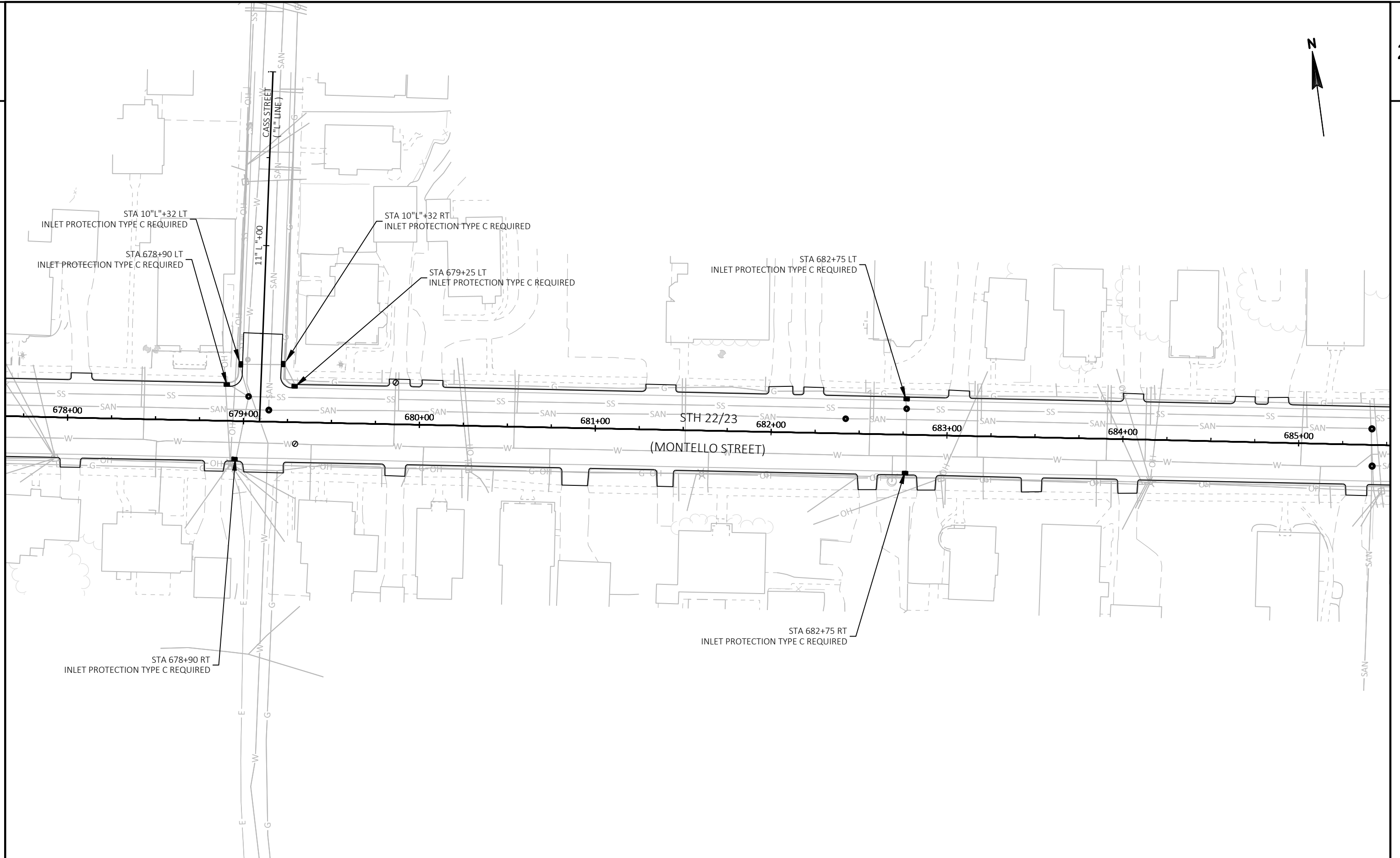
EROSION CONTROL

SHEET

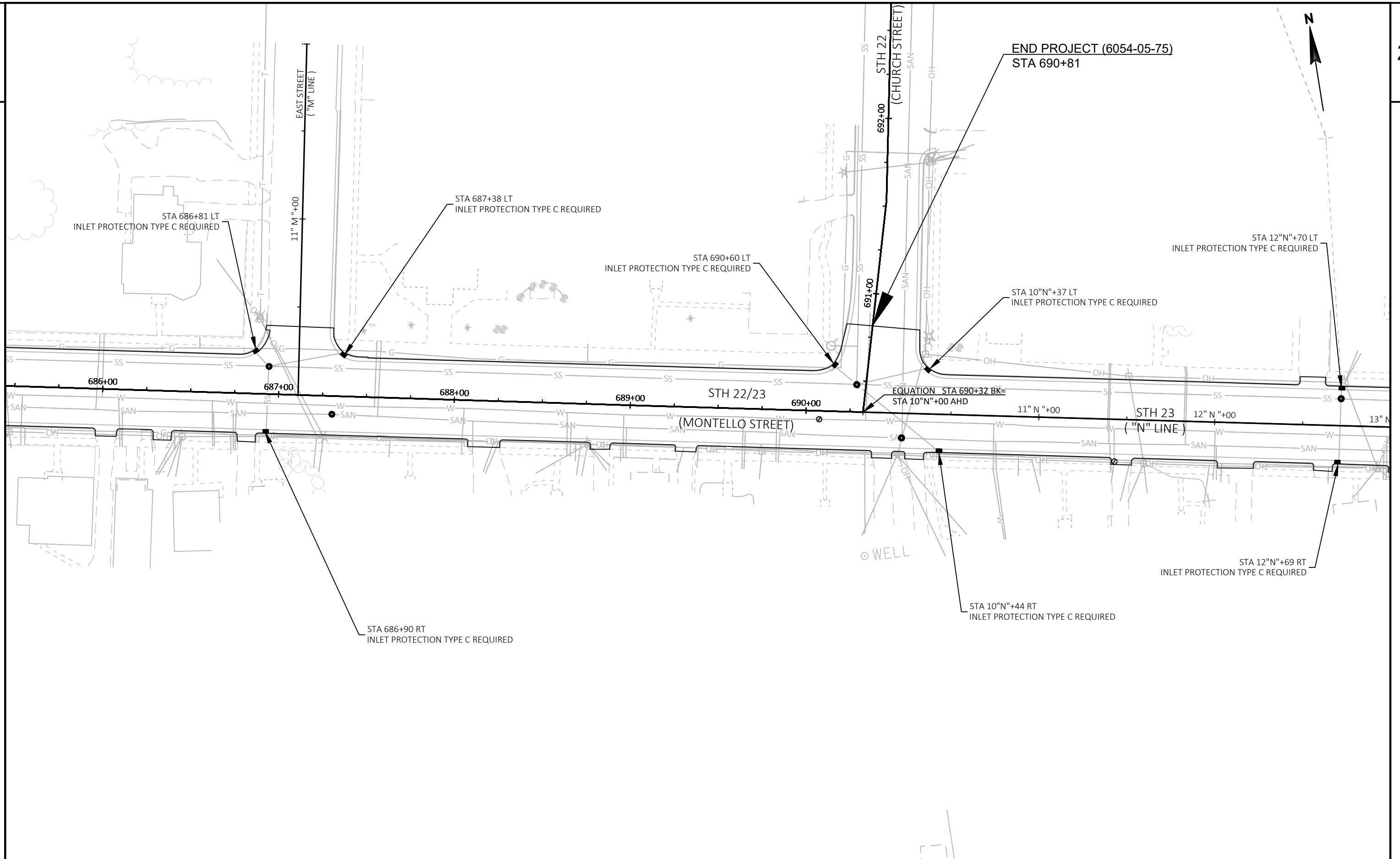
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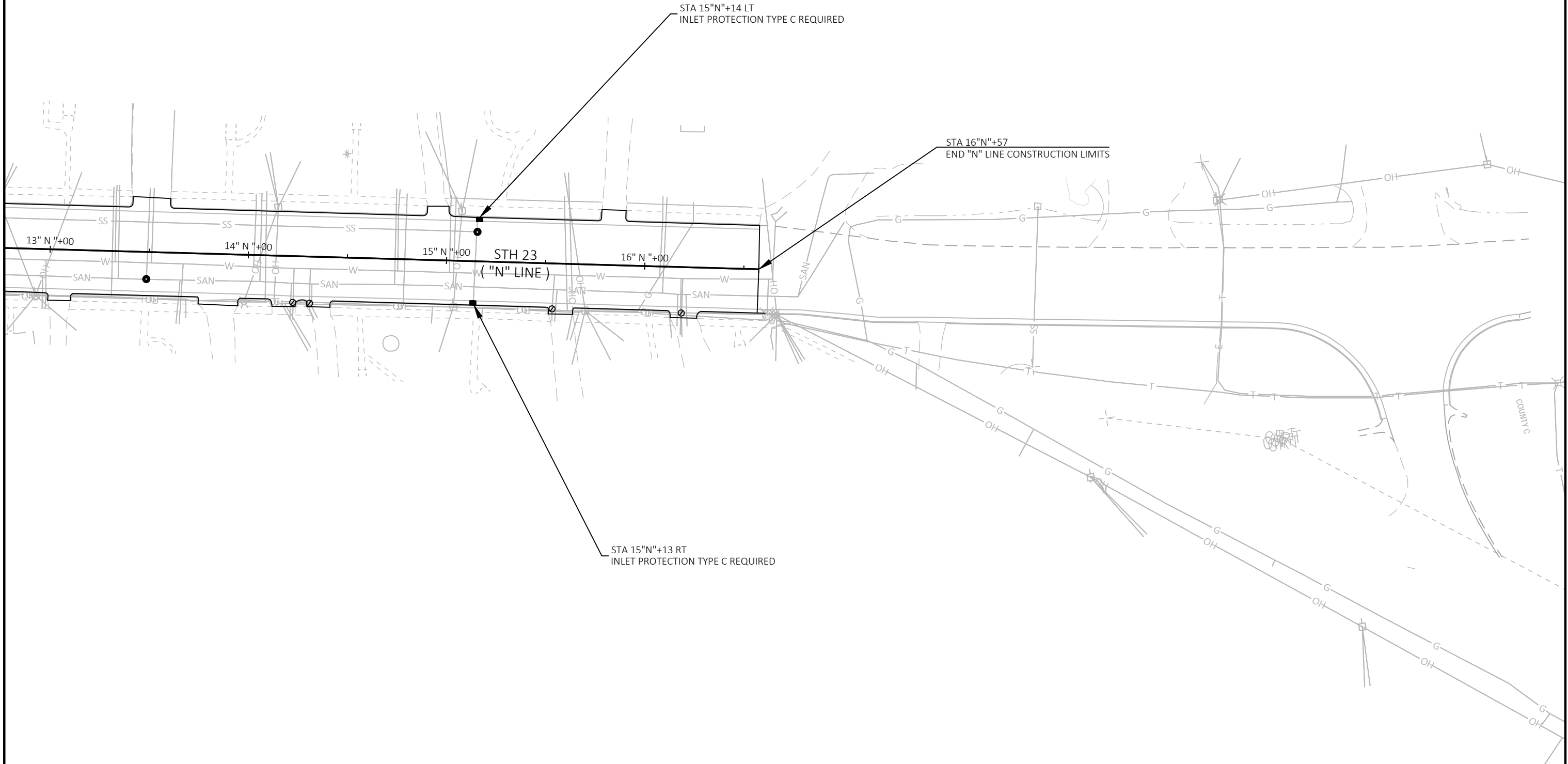
PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	EROSION CONTROL	SHEET	E
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PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	EROSION CONTROL	SHEET	E
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PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	EROSION CONTROL	SHEET	E
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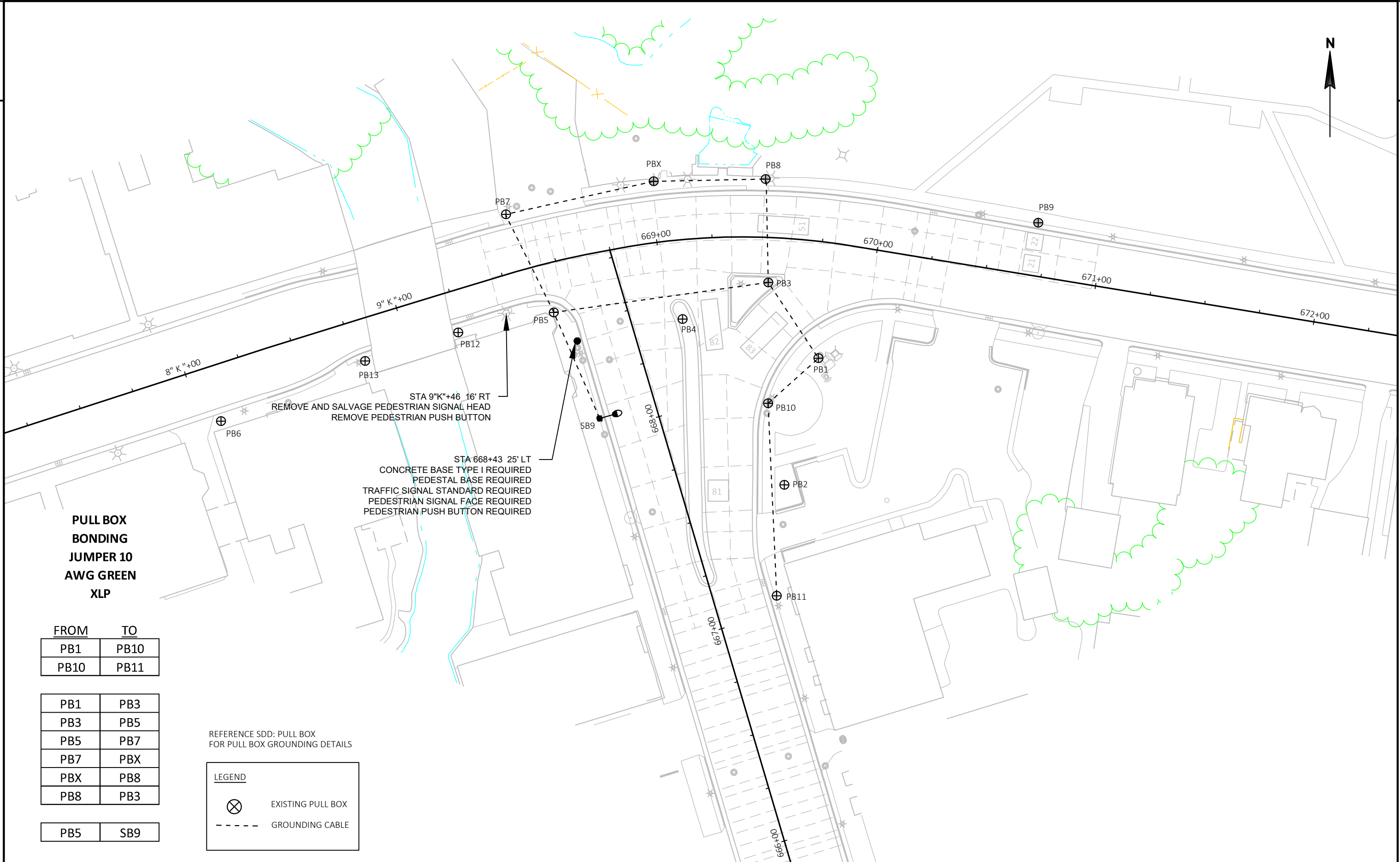


STA 15"N"+14 LT
INLET PROTECTION TYPE C REQUIRED

STA 16"N"+57
END "N" LINE CONSTRUCTION LIMITS

STA 15"N"+13 RT
INLET PROTECTION TYPE C REQUIRED

PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	EROSION CONTROL	SHEET	E
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**PULL BOX
BONDING
JUMPER 10
AWG GREEN
XLP**

FROM	TO
PB1	PB10
PB10	PB11

PB1	PB3
PB3	PB5
PB5	PB7
PB7	PBX
PBX	PB8
PB8	PB3



PB5	SB9
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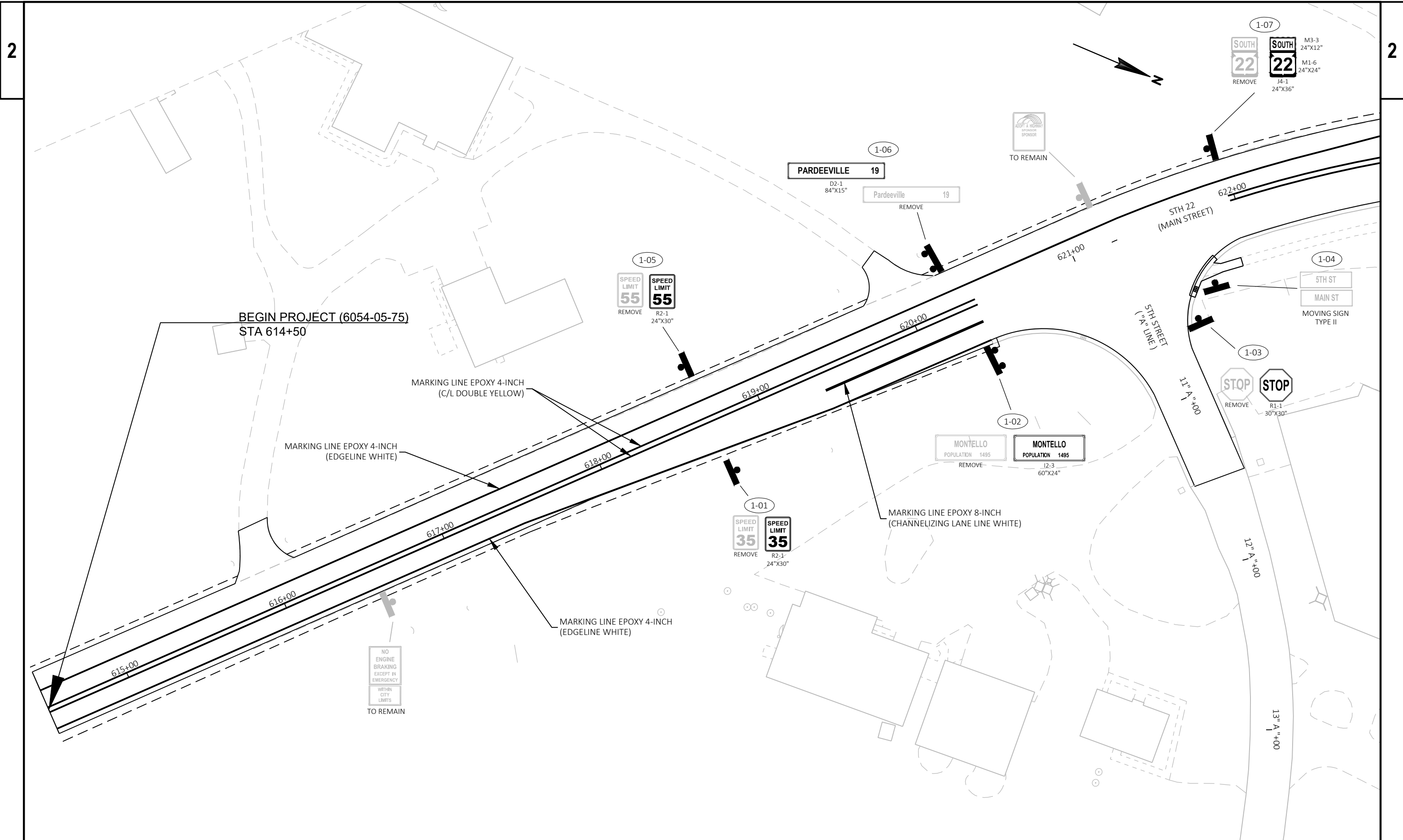
STA 9"K"+46 16' RT
REMOVE AND SALVAGE PEDESTRIAN SIGNAL HEAD
REMOVE PEDESTRIAN PUSH BUTTON

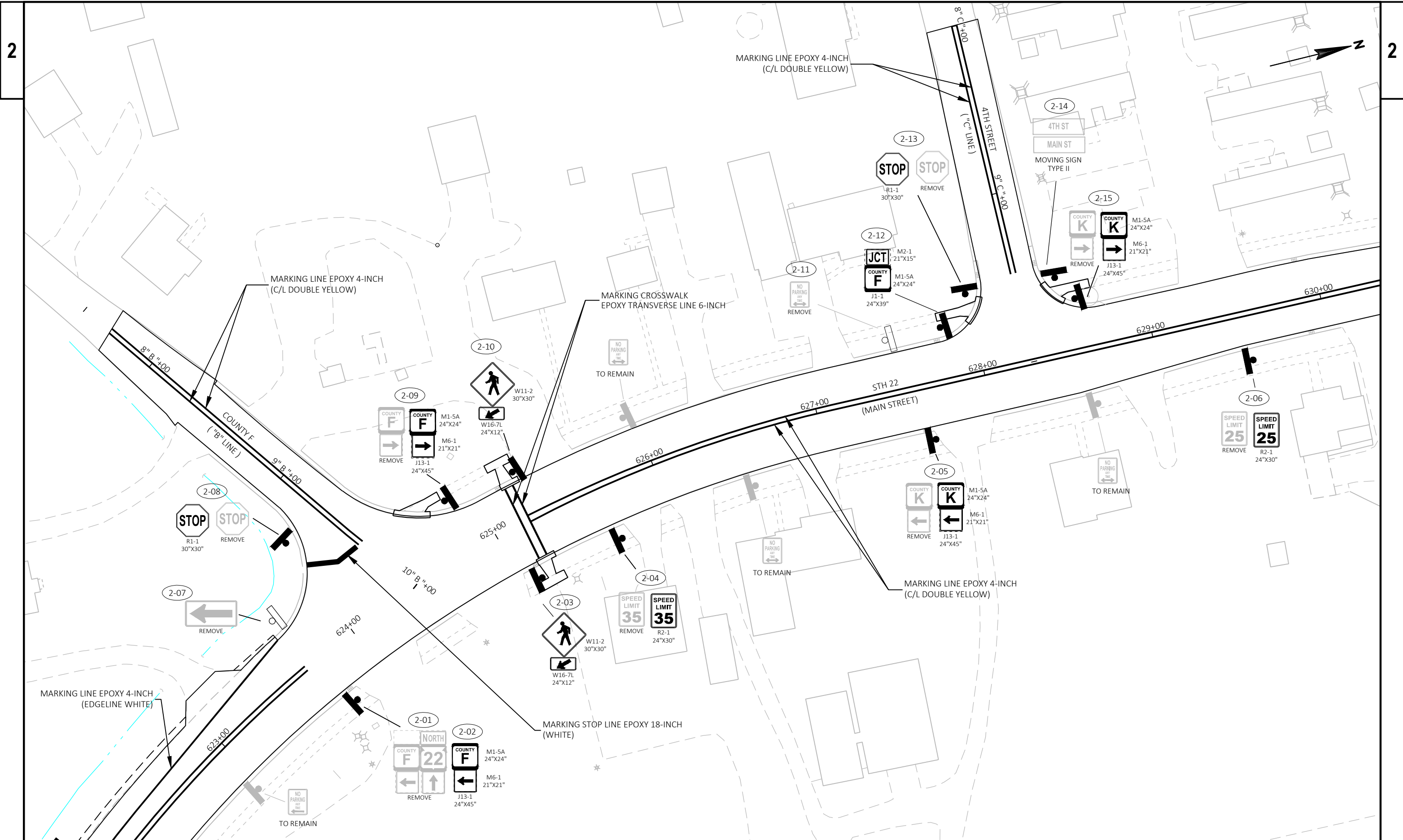
STA 668+43 25' LT
CONCRETE BASE TYPE I REQUIRED
PEDESTAL BASE REQUIRED
TRAFFIC SIGNAL STANDARD REQUIRED
PEDESTRIAN SIGNAL FACE REQUIRED
PEDESTRIAN PUSH BUTTON REQUIRED

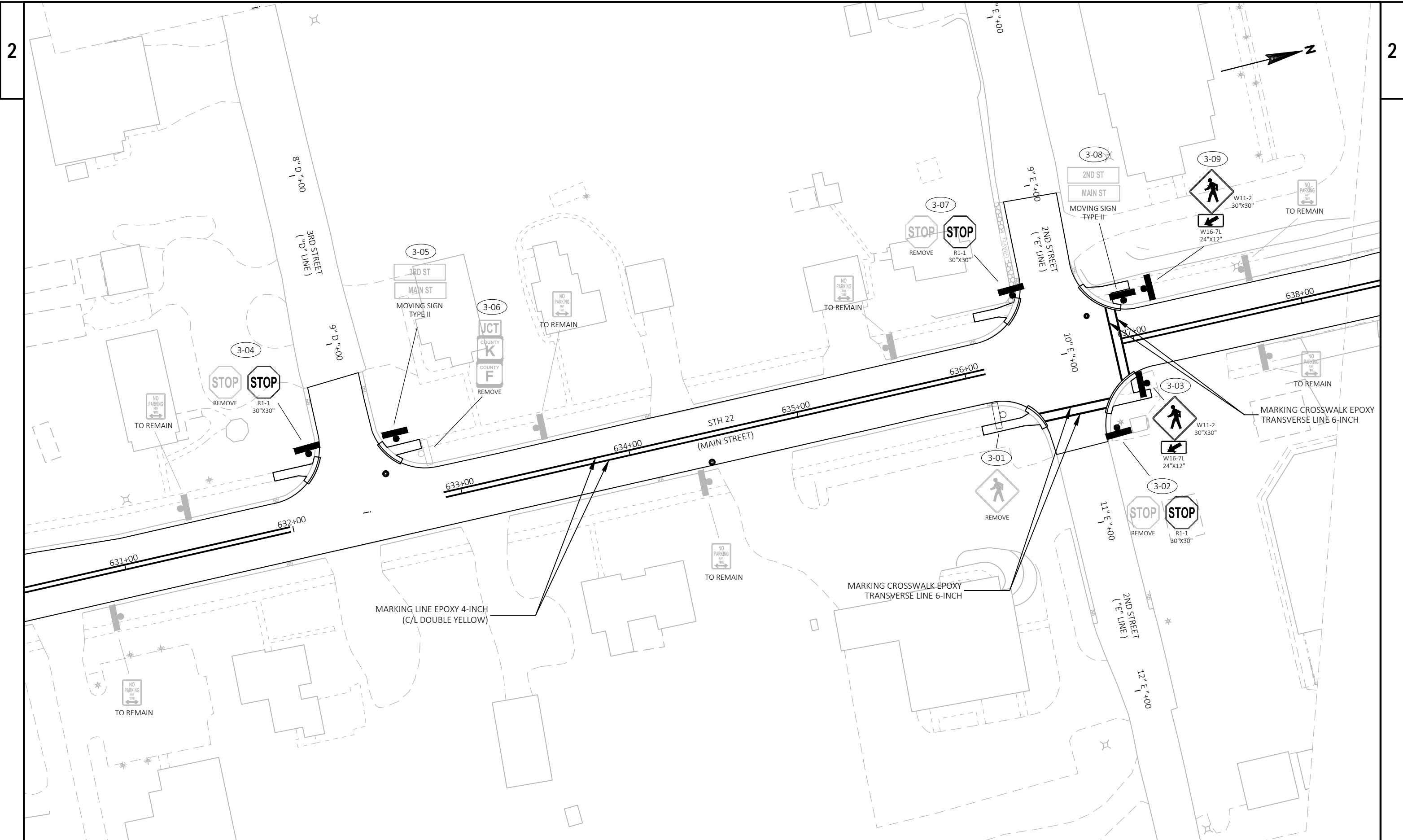
REFERENCE SDD: PULL BOX
FOR PULL BOX GROUNDING DETAILS

LEGEND

-  EXISTING PULL BOX
-  GROUNDING CABLE







PROJECT NO: 6054-05-75

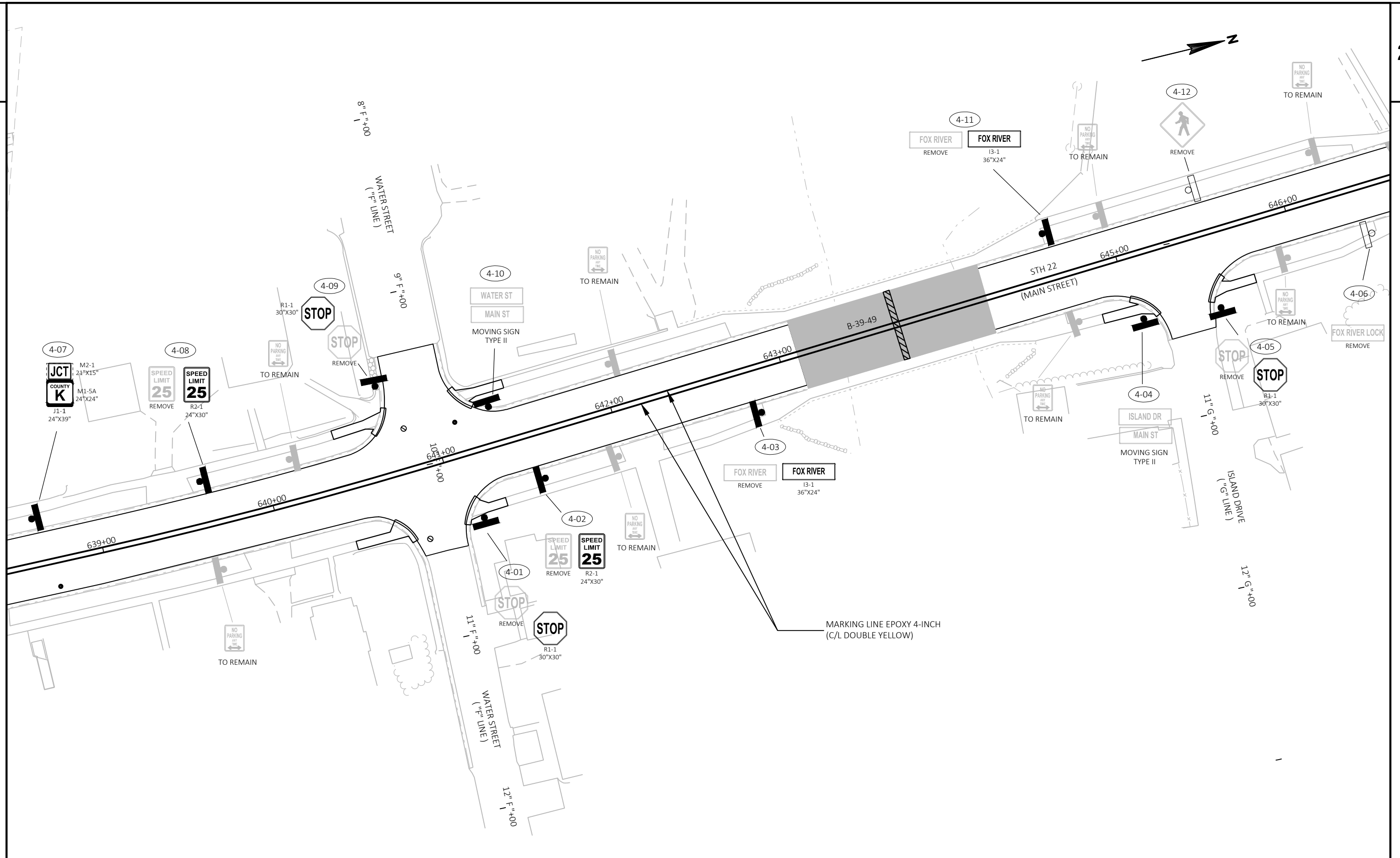
HWY: STH 22

COUNTY: MARQUETTE

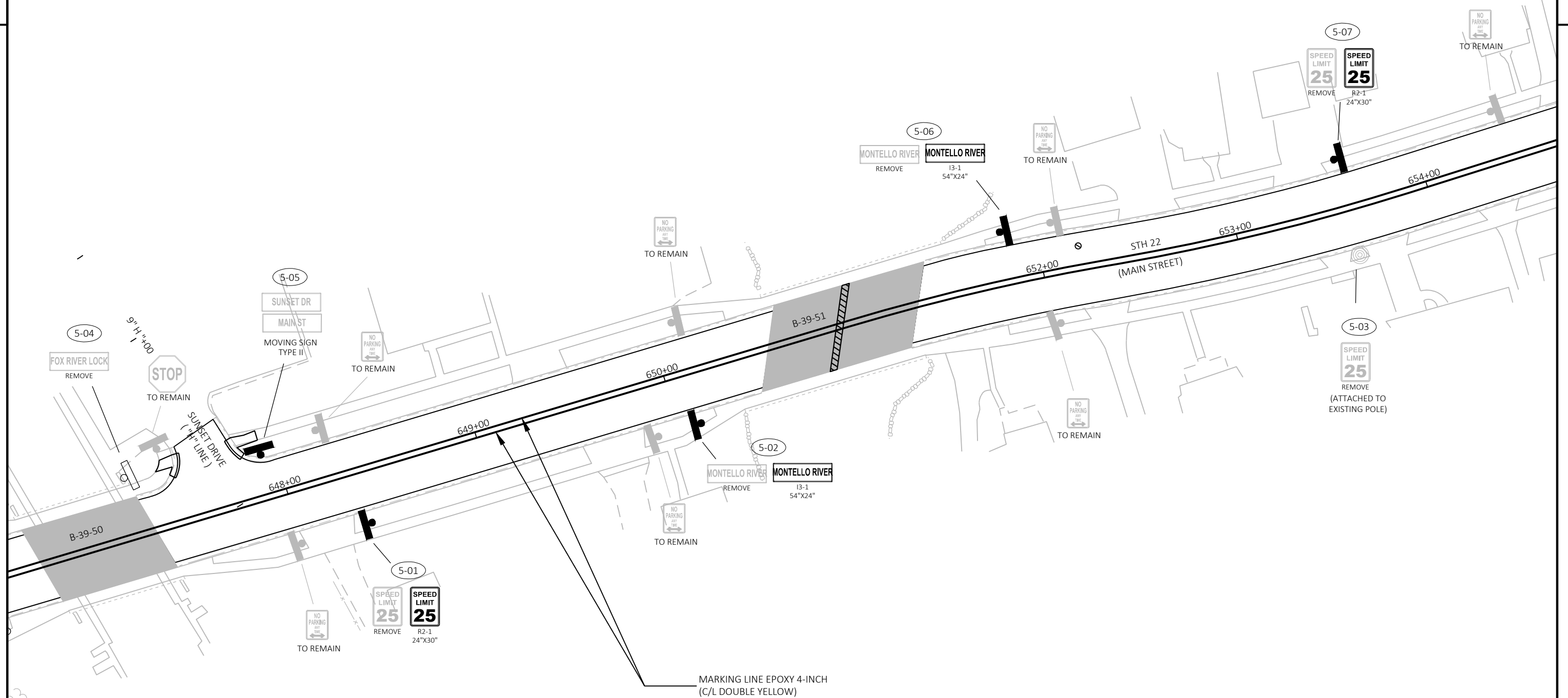
PERMANENT SIGNING / PAVEMENT MARKING

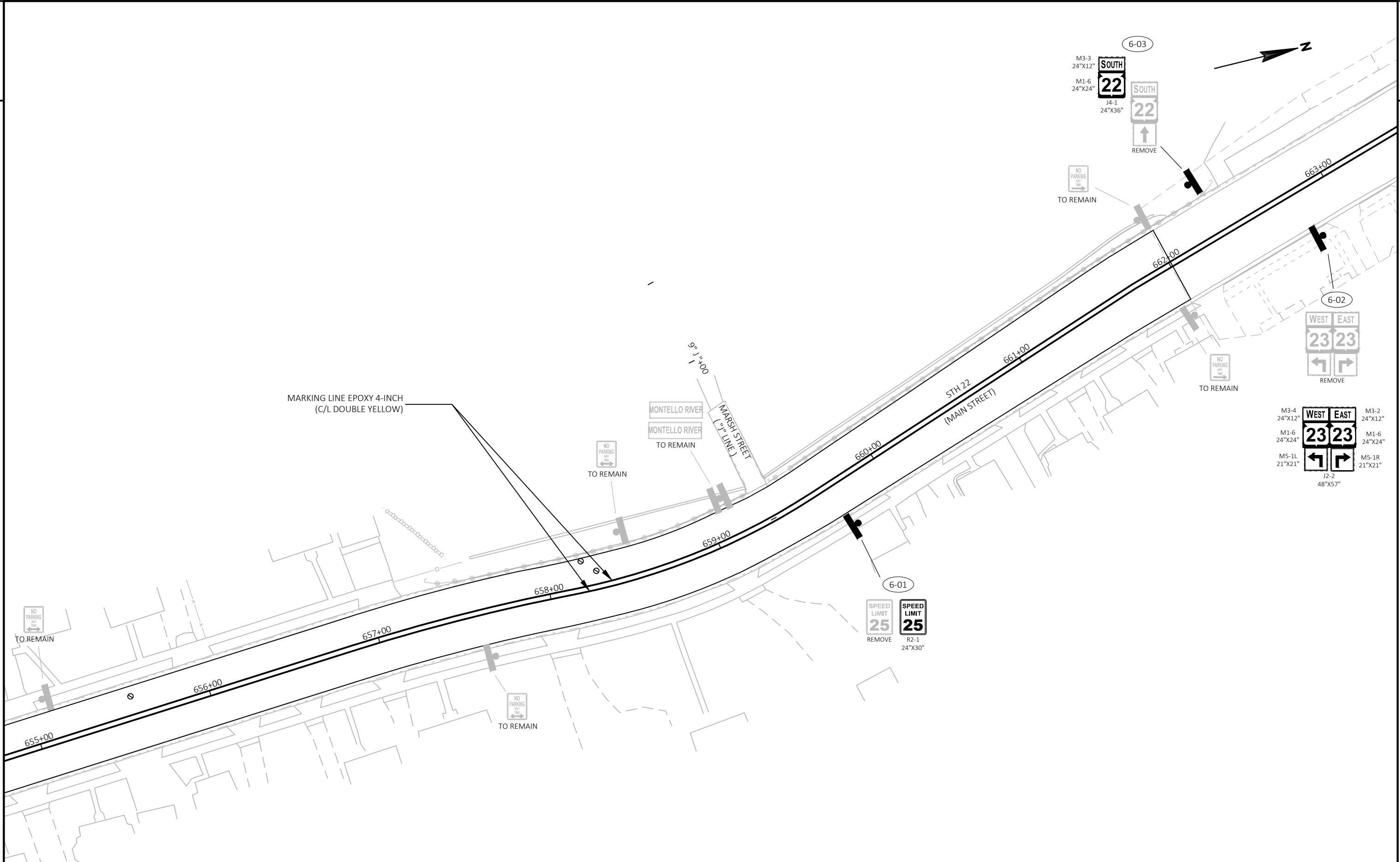
SHEET

E



PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	PERMANENT SIGNING / PAVEMENT MARKING	SHEET	E
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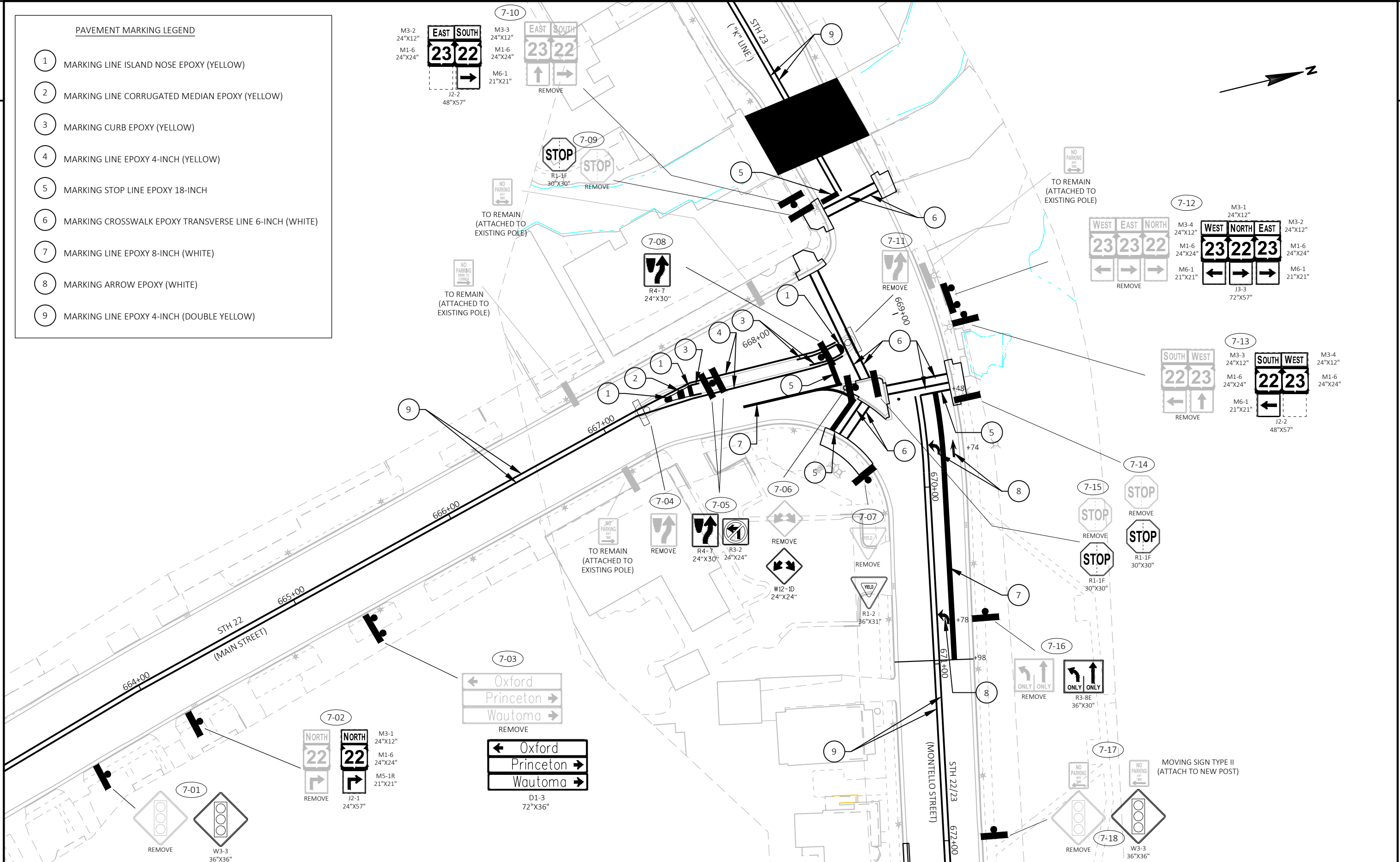


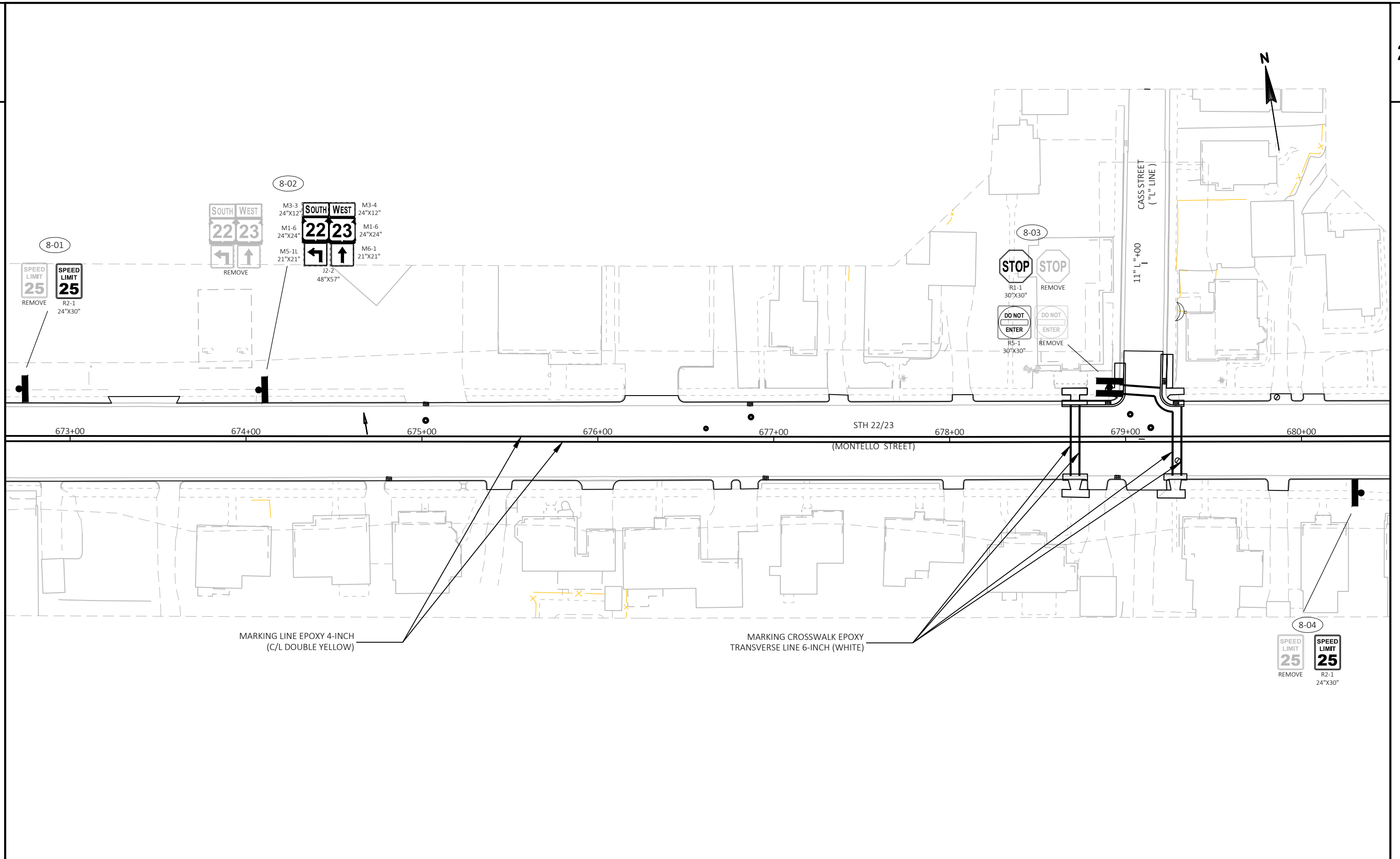


PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	PERMANENT SIGNING / PAVEMENT MARKING	SHEET	E
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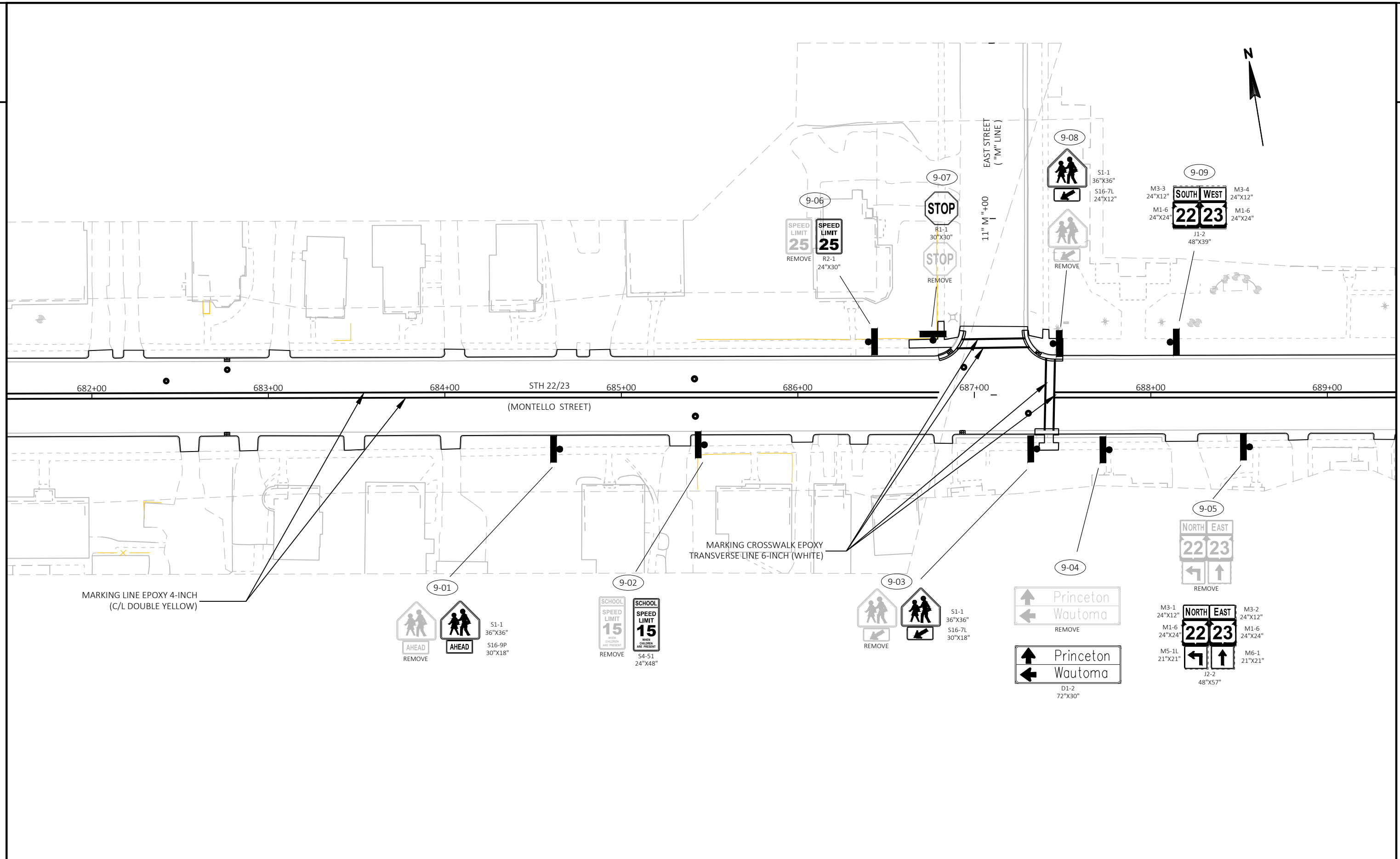
PAVEMENT MARKING LEGEND

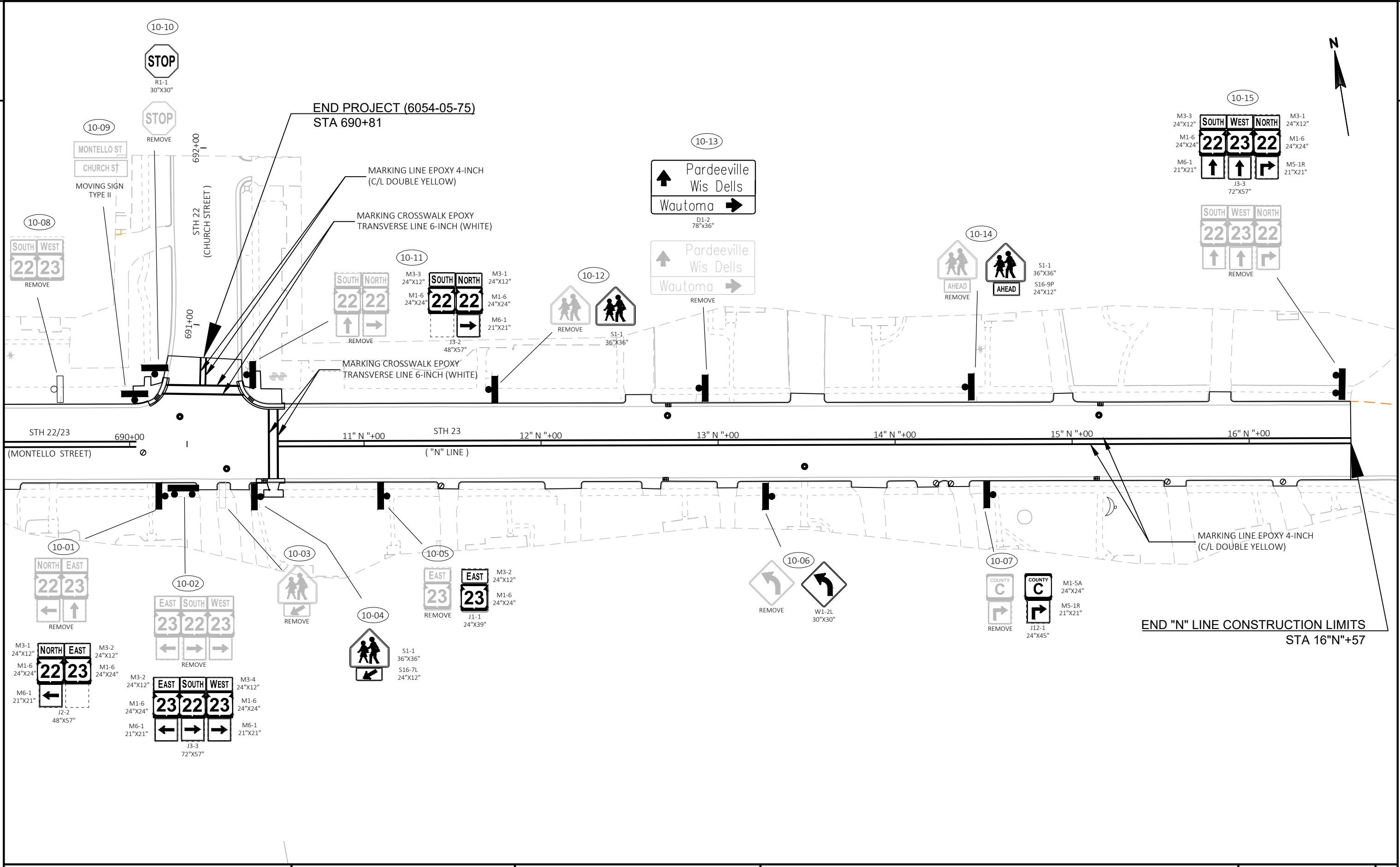
- 1 MARKING LINE ISLAND NOSE EPOXY (YELLOW)
- 2 MARKING LINE CORRUGATED MEDIAN EPOXY (YELLOW)
- 3 MARKING CURB EPOXY (YELLOW)
- 4 MARKING LINE EPOXY 4-INCH (YELLOW)
- 5 MARKING STOP LINE EPOXY 18-INCH
- 6 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 7 MARKING LINE EPOXY 8-INCH (WHITE)
- 8 MARKING ARROW EPOXY (WHITE)
- 9 MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)

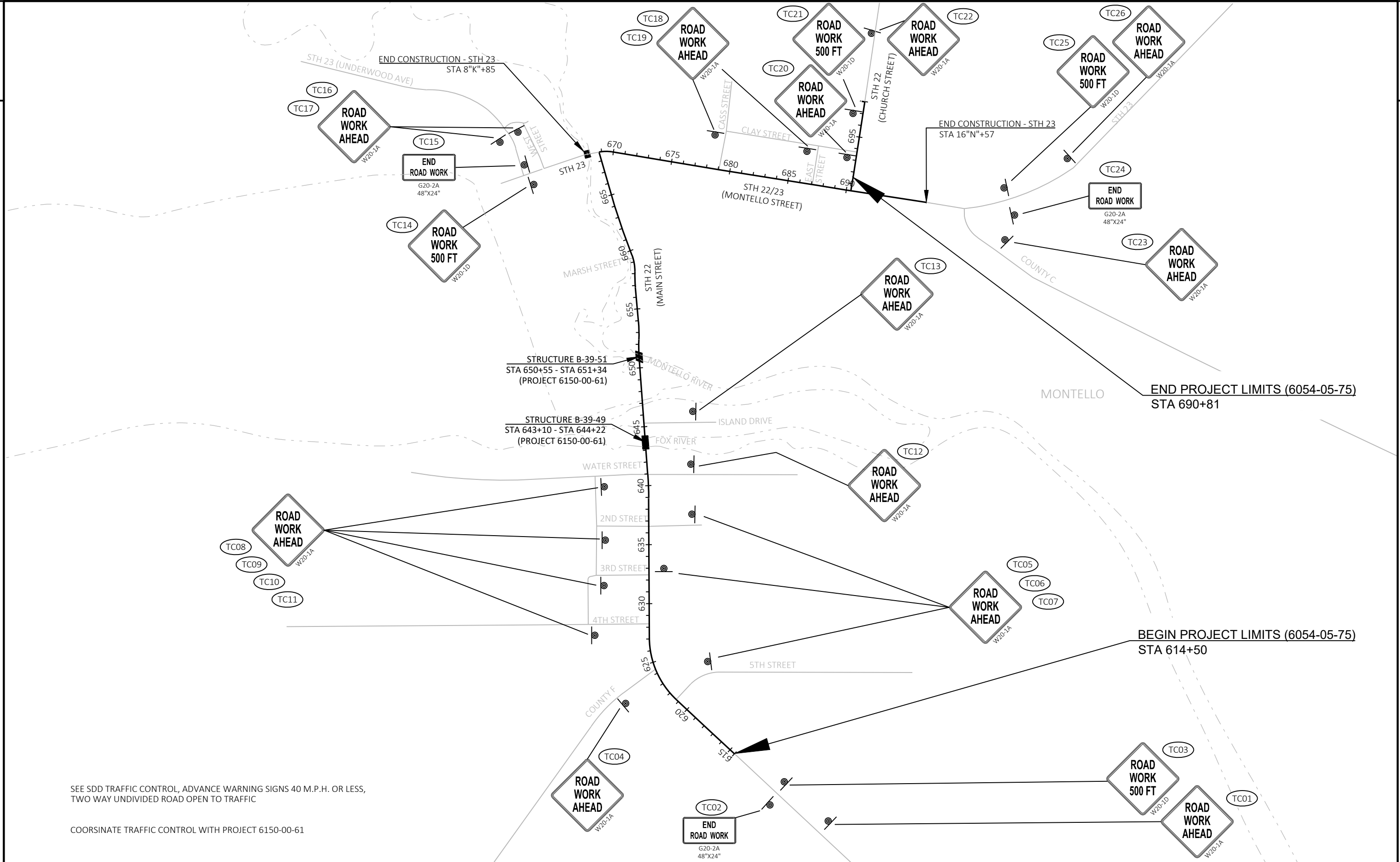




PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	PERMANENT SIGNING / PAVEMENT MARKING	SHEET	E
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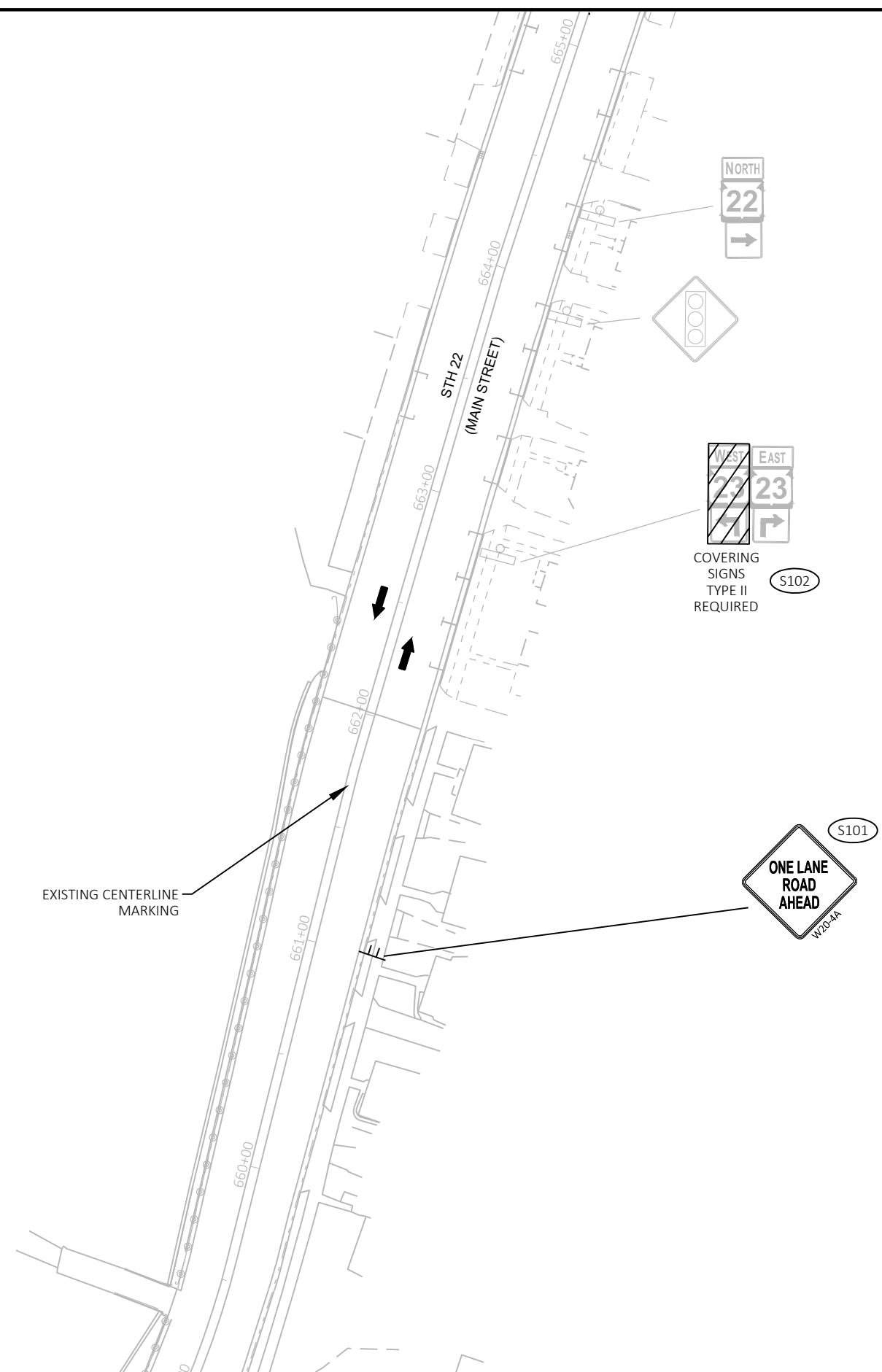
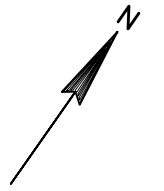







SEE SDD TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS,
TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC

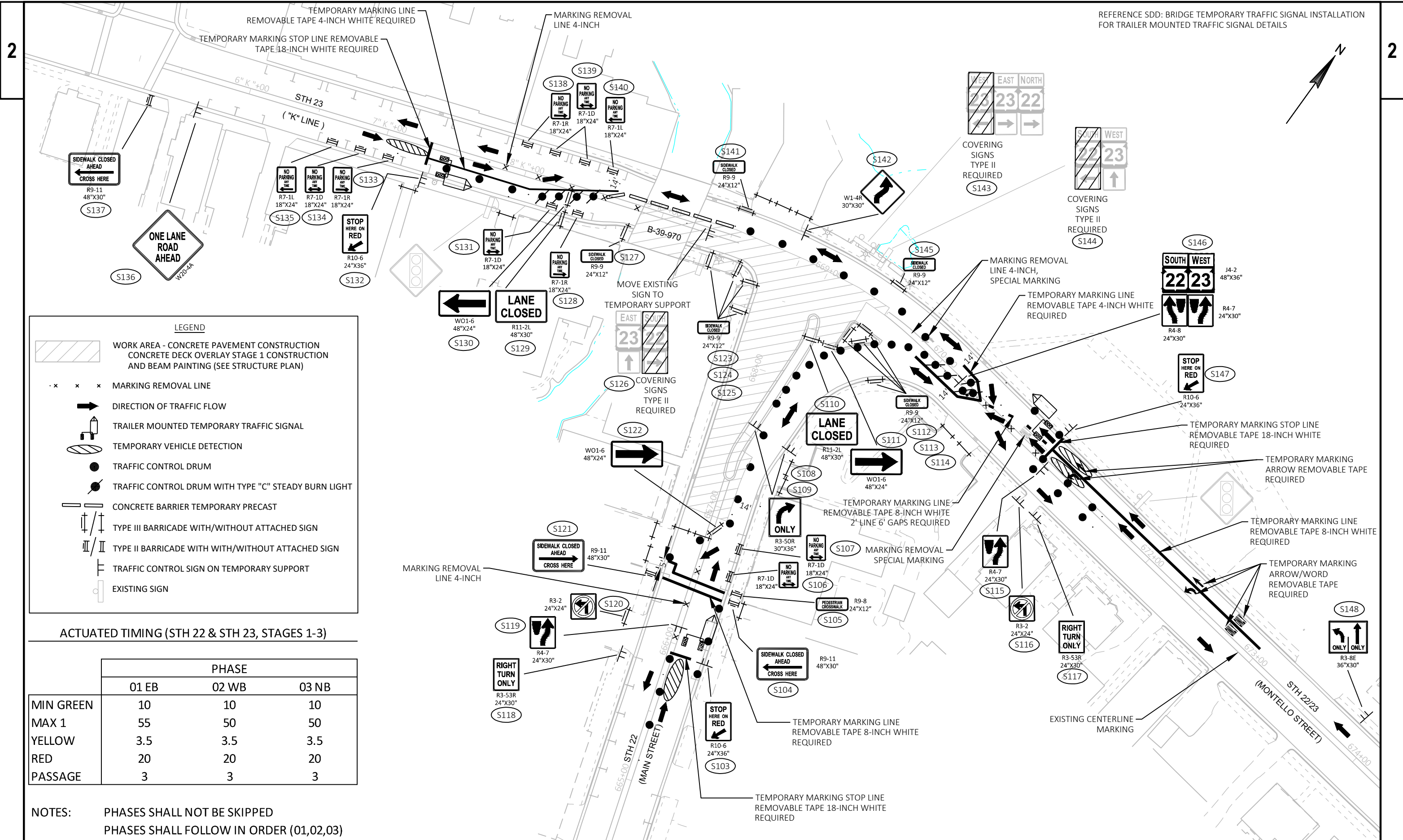
COORDINATE TRAFFIC CONTROL WITH PROJECT 6150-00-61

PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	TRAFFIC CONTROL	SHEET	E
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LEGEND

-  DIRECTION OF TRAFFIC FLOW
-  TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
-  EXISTING SIGN



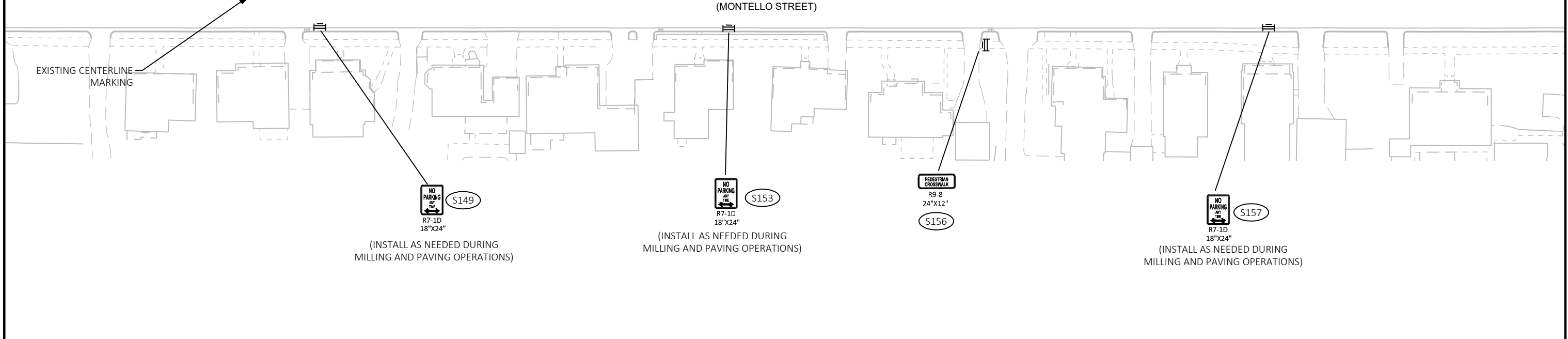
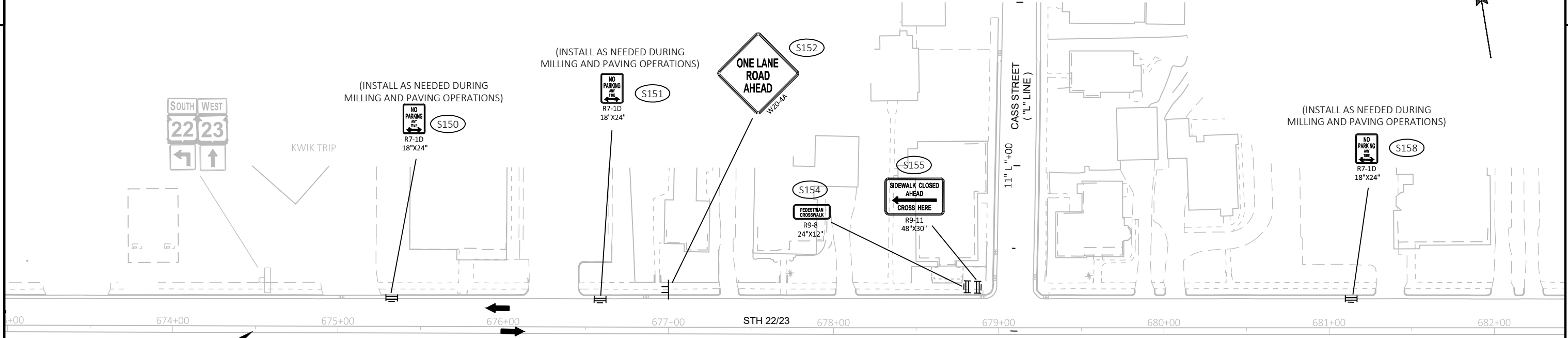
LEGEND

- WORK AREA - CONCRETE PAVEMENT CONSTRUCTION
CONCRETE DECK OVERLAY STAGE 1 CONSTRUCTION
AND BEAM PAINTING (SEE STRUCTURE PLAN)
- MARKING REMOVAL LINE
- DIRECTION OF TRAFFIC FLOW
- TRAILER MOUNTED TEMPORARY TRAFFIC SIGNAL
- TEMPORARY VEHICLE DETECTION
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- CONCRETE BARRIER TEMPORARY PRECAST
- TYPE III BARRICADE WITH/WITHOUT ATTACHED SIGN
- TYPE II BARRICADE WITH WITH/WITHOUT ATTACHED SIGN
- TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
- EXISTING SIGN

ACTUATED TIMING (STH 22 & STH 23, STAGES 1-3)




	PHASE		
	01 EB	02 WB	03 NB
MIN GREEN	10	10	10
MAX 1	55	50	50
YELLOW	3.5	3.5	3.5
RED	20	20	20
PASSAGE	3	3	3

NOTES: PHASES SHALL NOT BE SKIPPED
PHASES SHALL FOLLOW IN ORDER (01,02,03)

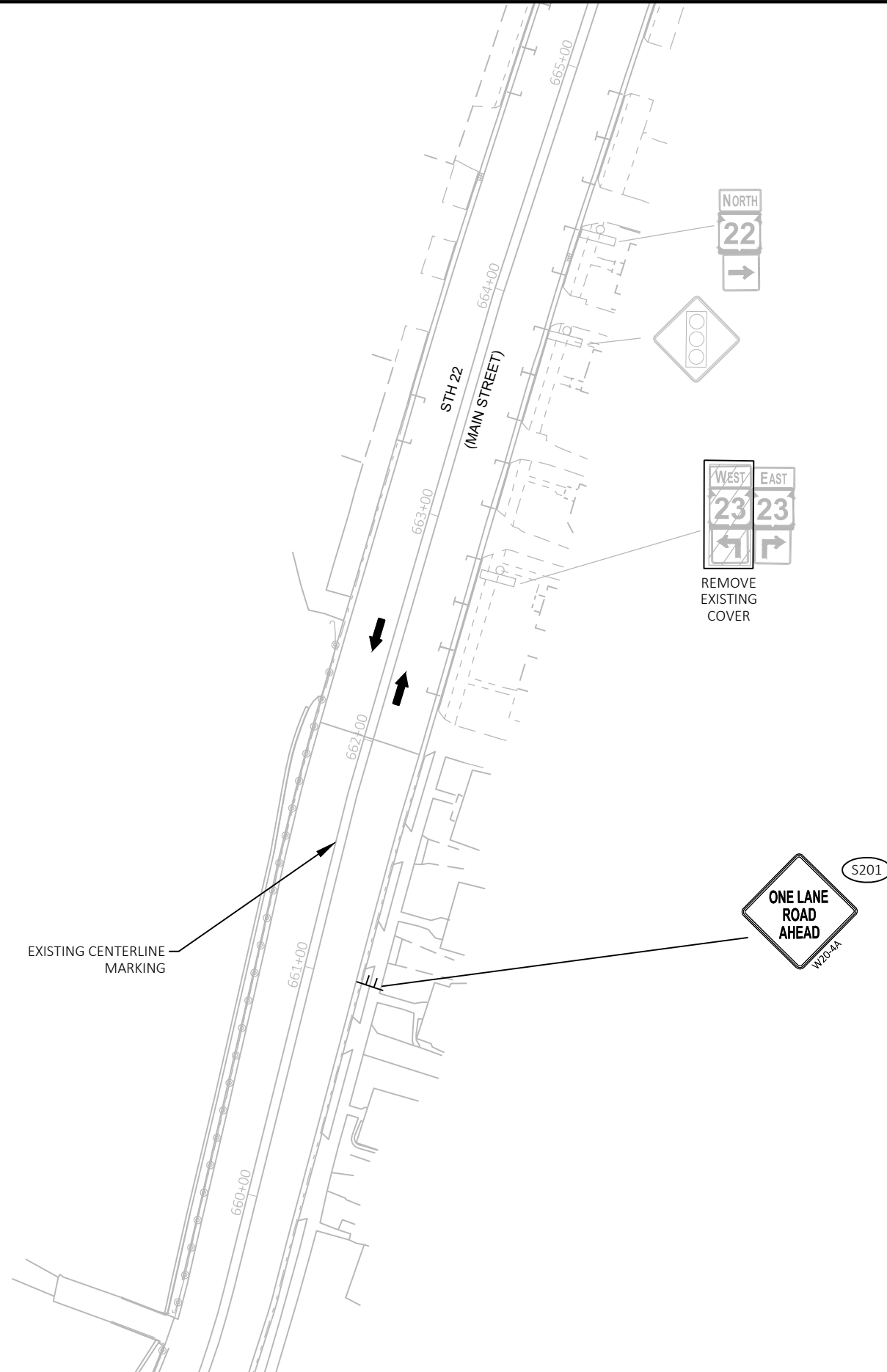
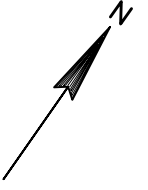





SDD TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

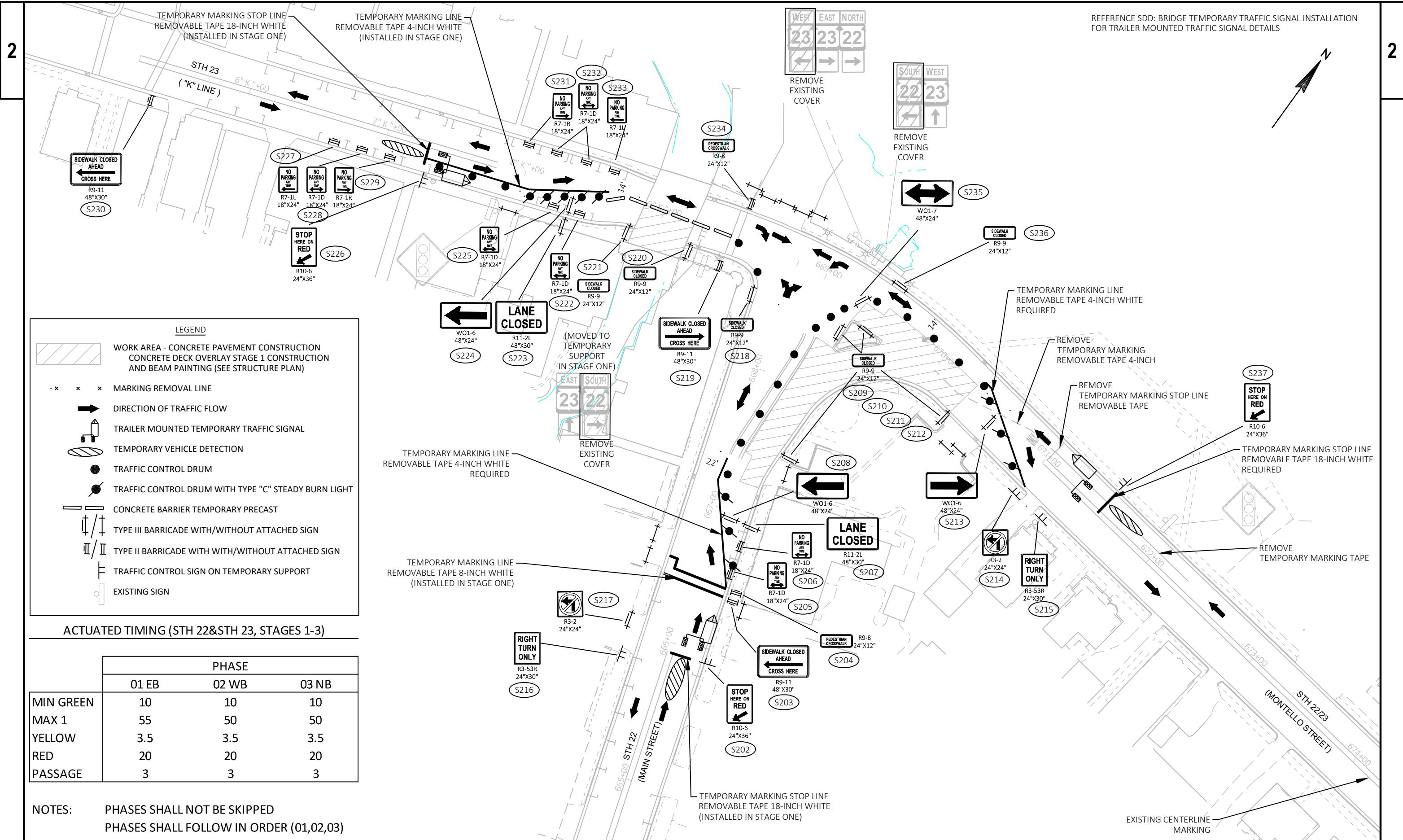
LEGEND

-  DIRECTION OF TRAFFIC FLOW
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  EXISTING SIGN

PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	TRAFFIC CONTROL - STAGE ONE	SHEET	E
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- LEGEND**
-  DIRECTION OF TRAFFIC FLOW
 -  TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
 -  EXISTING SIGN



REFERENCE SDD: BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION FOR TRAILER MOUNTED TRAFFIC SIGNAL DETAILS

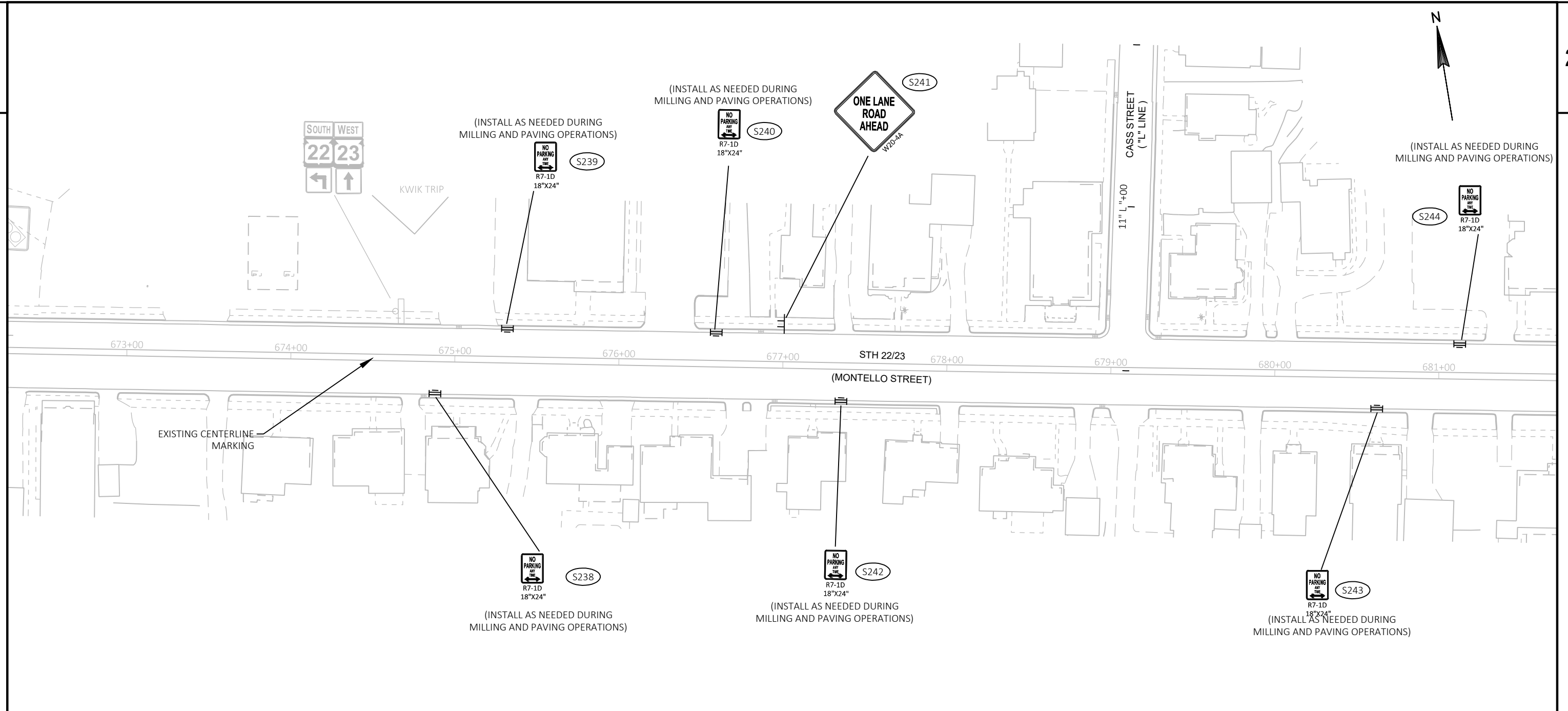
LEGEND

- WORK AREA - CONCRETE PAVEMENT CONSTRUCTION CONCRETE DECK OVERLAY STAGE 1 CONSTRUCTION AND BEAM PAINTING (SEE STRUCTURE PLAN)
- MARKING REMOVAL LINE
- DIRECTION OF TRAFFIC FLOW
- TRAILER MOUNTED TEMPORARY TRAFFIC SIGNAL
- TEMPORARY VEHICLE DETECTION
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- CONCRETE BARRIER TEMPORARY PRECAST
- TYPE III BARRICADE WITH/WITHOUT ATTACHED SIGN
- TYPE II BARRICADE WITH WITH/WITHOUT ATTACHED SIGN
- TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
- EXISTING SIGN

ACTUATED TIMING (STH 22&STH 23, STAGES 1-3)

	PHASE		
	01 EB	02 WB	03 NB
MIN GREEN	10	10	10
MAX 1	55	50	50
YELLOW	3.5	3.5	3.5
RED	20	20	20
PASSAGE	3	3	3

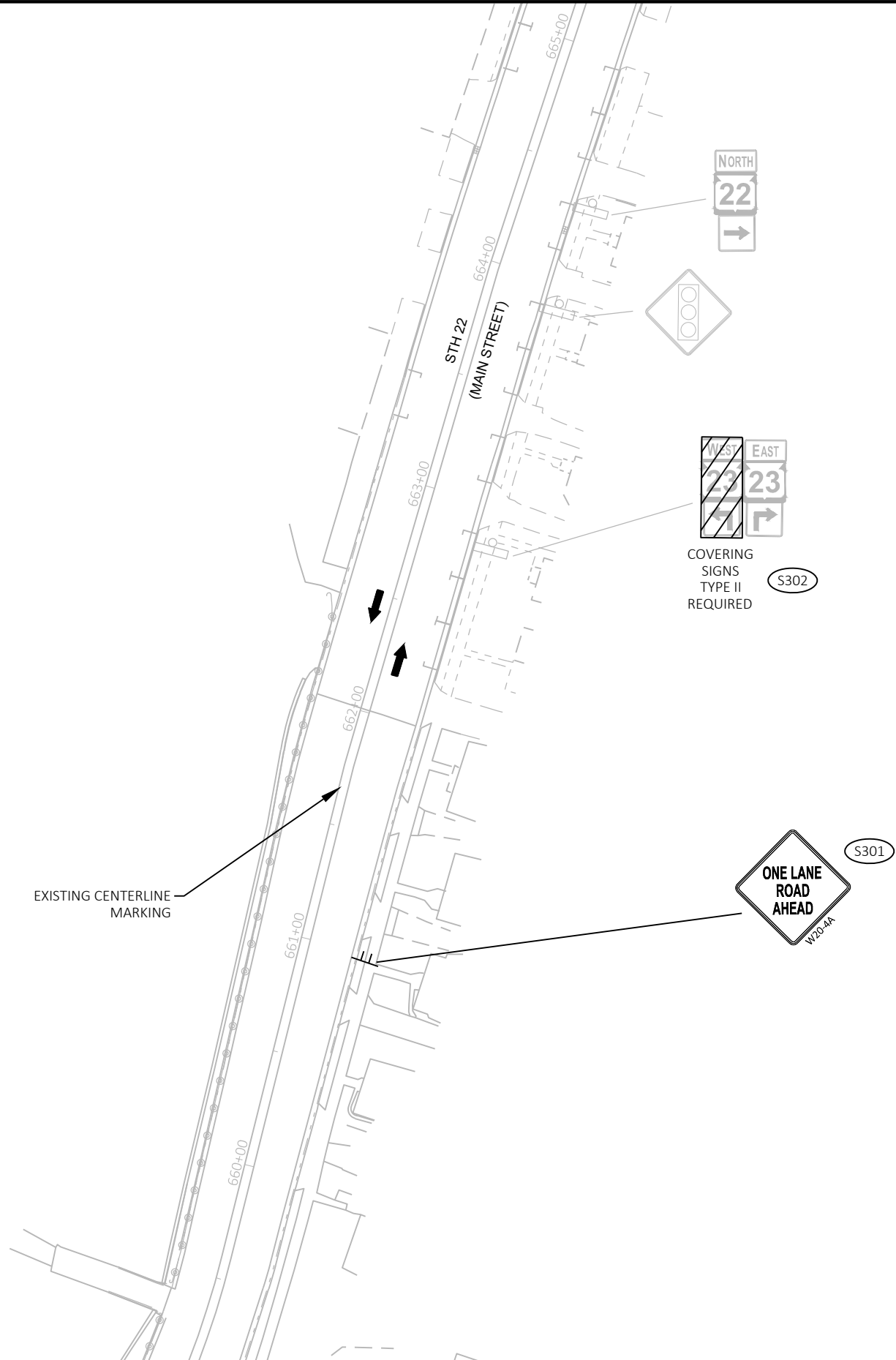
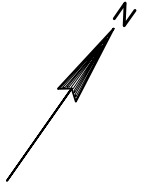
NOTES: PHASES SHALL NOT BE SKIPPED
PHASES SHALL FOLLOW IN ORDER (01,02,03)






SDD TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

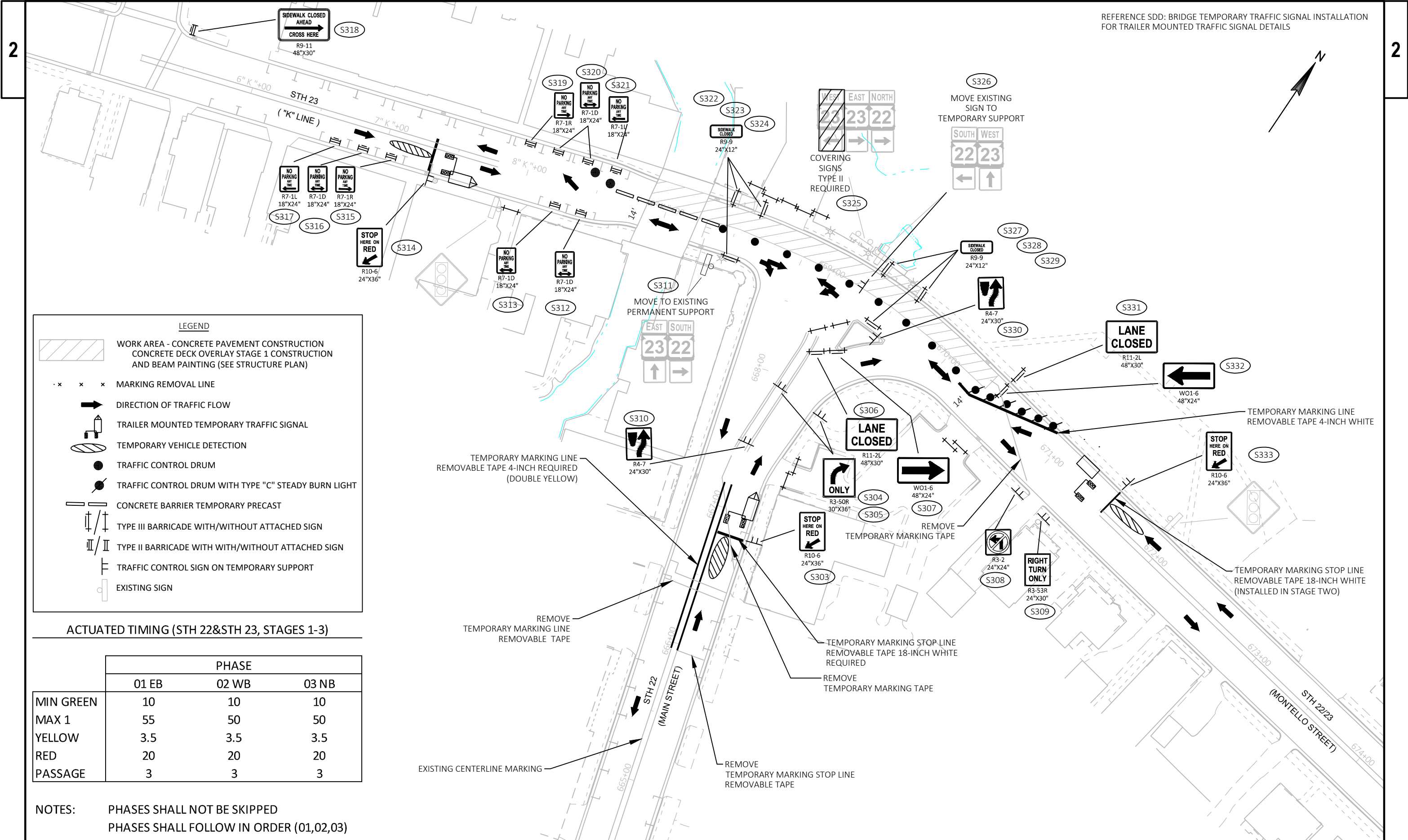
LEGEND

- DIRECTION OF TRAFFIC FLOW
- TYPE II BARRICADE WITH ATTACHED SIGN
- EXISTING SIGN



- LEGEND**
-  DIRECTION OF TRAFFIC FLOW
 -  TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
 -  EXISTING SIGN

PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	TRAFFIC CONTROL - STAGE THREE	SHEET	E
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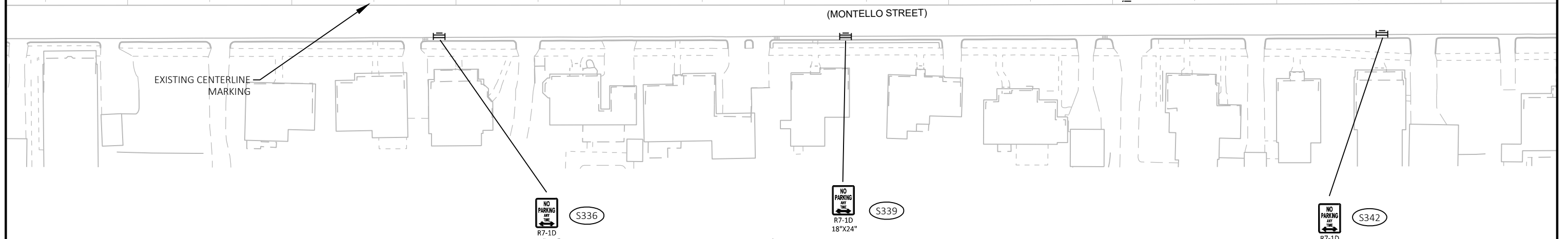
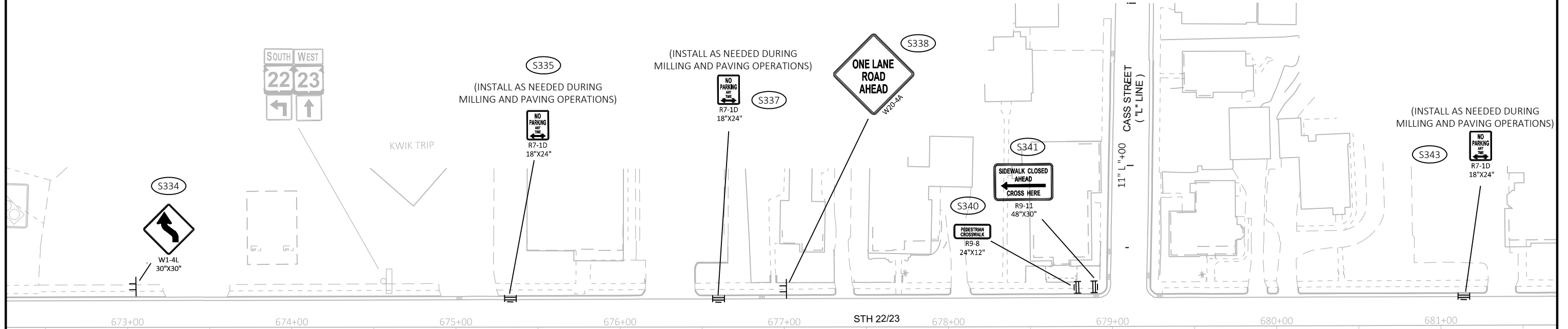
LEGEND

- WORK AREA - CONCRETE PAVEMENT CONSTRUCTION
CONCRETE DECK OVERLAY STAGE 1 CONSTRUCTION
AND BEAM PAINTING (SEE STRUCTURE PLAN)
- MARKING REMOVAL LINE
- DIRECTION OF TRAFFIC FLOW
- TRAILER MOUNTED TEMPORARY TRAFFIC SIGNAL
- TEMPORARY VEHICLE DETECTION
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- CONCRETE BARRIER TEMPORARY PRECAST
- TYPE III BARRICADE WITH/WITHOUT ATTACHED SIGN
- TYPE II BARRICADE WITH WITH/WITHOUT ATTACHED SIGN
- TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
- EXISTING SIGN

ACTUATED TIMING (STH 22&STH 23, STAGES 1-3)




	PHASE		
	01 EB	02 WB	03 NB
MIN GREEN	10	10	10
MAX 1	55	50	50
YELLOW	3.5	3.5	3.5
RED	20	20	20
PASSAGE	3	3	3

NOTES: PHASES SHALL NOT BE SKIPPED
PHASES SHALL FOLLOW IN ORDER (01,02,03)

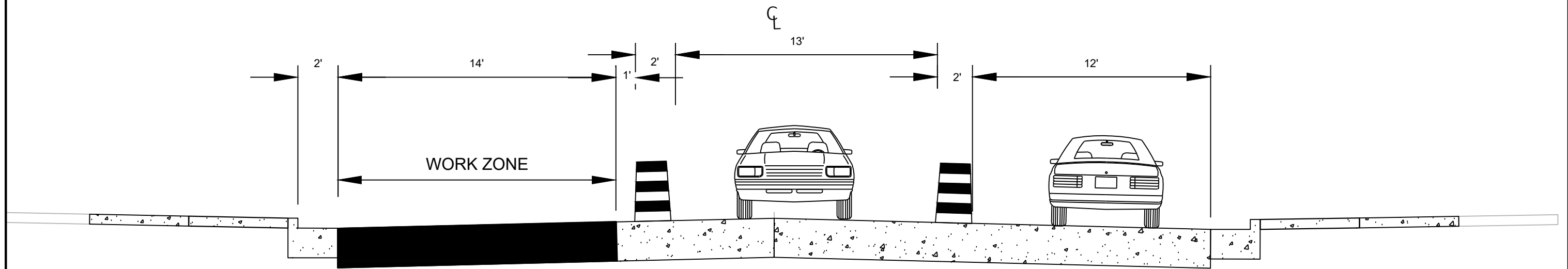


SDD TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

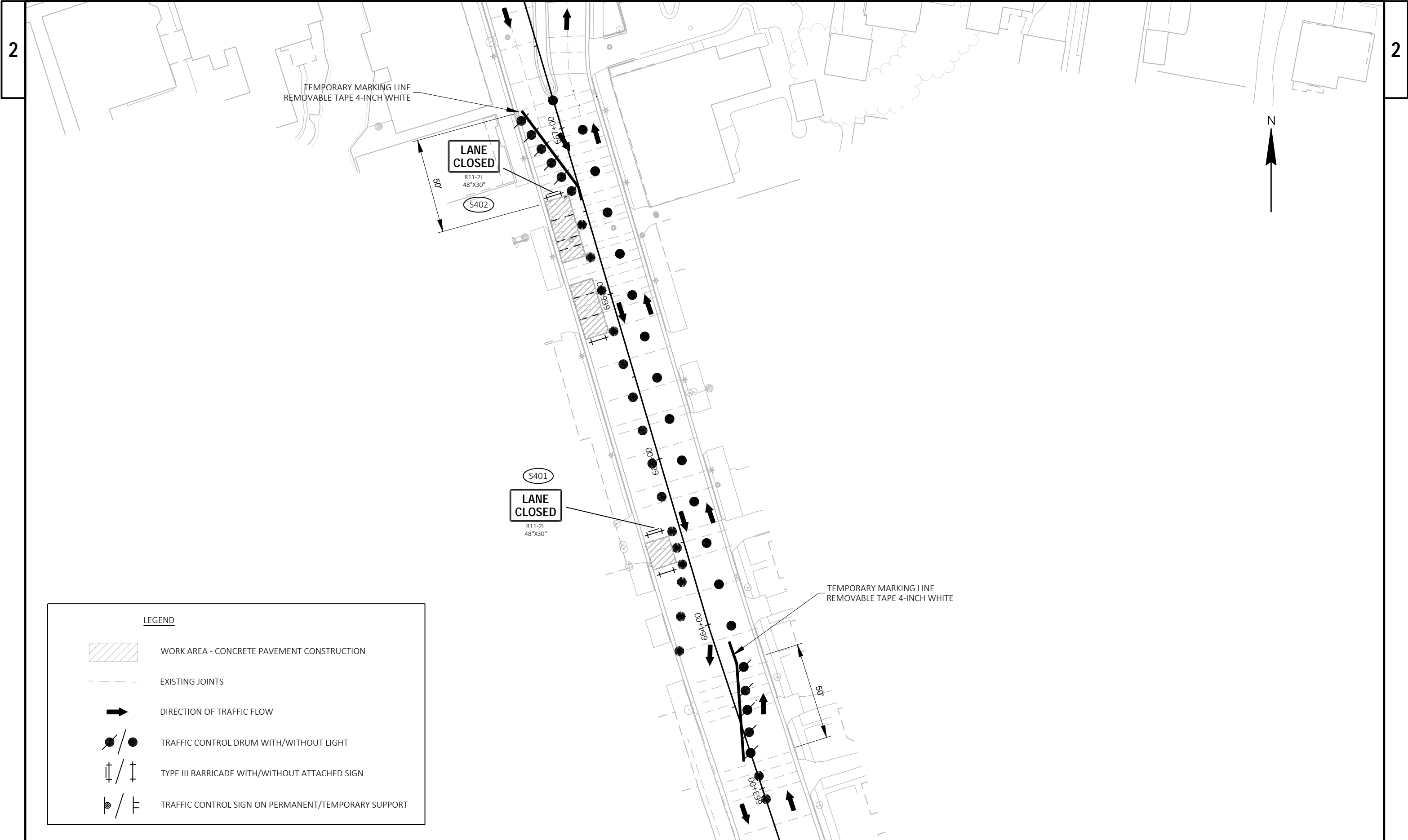
LEGEND

-  DIRECTION OF TRAFFIC FLOW
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  EXISTING SIGN

PROJECT NO: 6054-05-75	HWY: STH 22	COUNTY: MARQUETTE	TRAFFIC CONTROL - STAGE THREE	SHEET	E
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

TRAFFIC CONTROL - STAGE FOUR
 STH 22
 STA 664+40 - STA 666+62



2

2

LEGEND

-  WORK AREA - CONCRETE PAVEMENT CONSTRUCTION
-  EXISTING JOINTS
-  DIRECTION OF TRAFFIC FLOW
-  TRAFFIC CONTROL DRUM WITH/WITHOUT LIGHT
-  TYPE III BARRICADE WITH/WITHOUT ATTACHED SIGN
-  TRAFFIC CONTROL SIGN ON PERMANENT/TEMPORARY SUPPORT

PROJECT NO: 6054-05-75

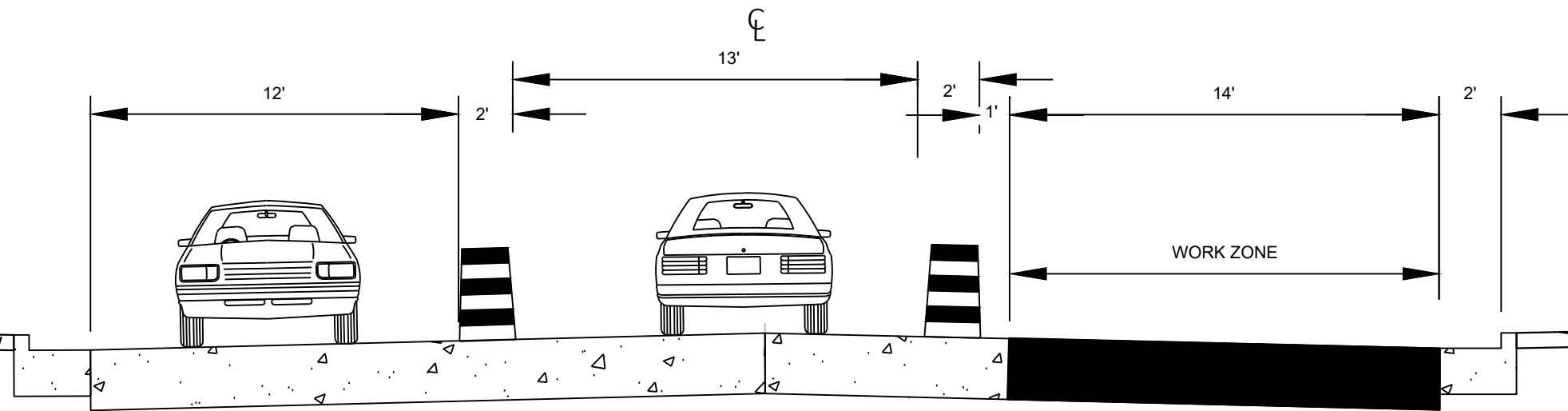
HWY: STH 22

COUNTY: MARQUETTE

TRAFFIC CONTROL - STAGE FOUR

SHEET

E



TRAFFIC CONTROL - STAGE FIVE
 STH 22
 STA 662+00 - STA 664+50









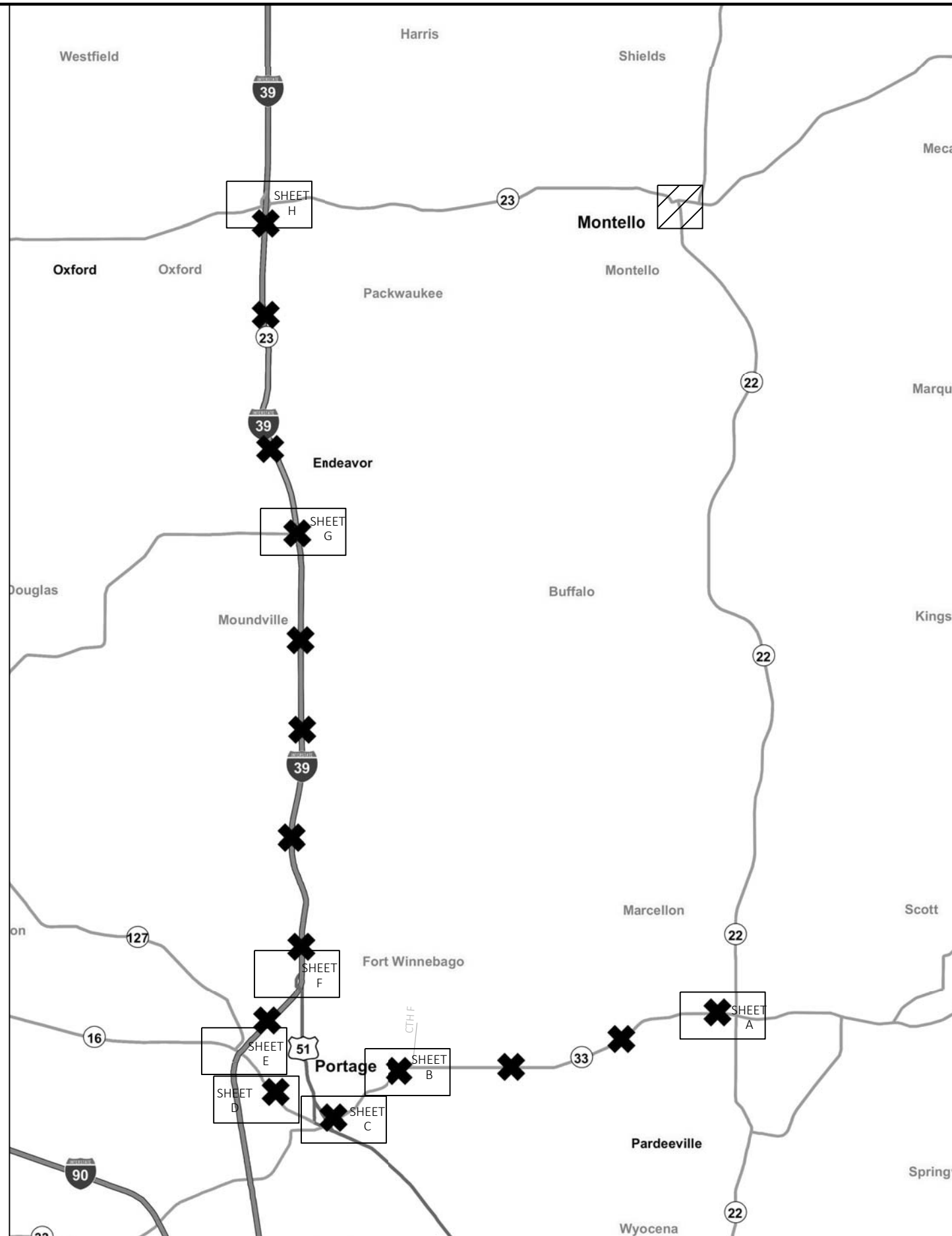
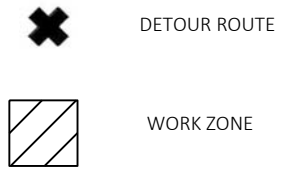
TEMPORARY MARKING LINE
REMOVABLE TAPE 4-INCH WHITE

S501
**LANE
CLOSED**
R11-2L
48"x30"

TEMPORARY MARKING LINE
REMOVABLE TAPE 4-INCH WHITE

LEGEND

-  WORK AREA - CONCRETE PAVEMENT CONSTRUCTION
-  EXISTING JOINTS
-  DIRECTION OF TRAFFIC FLOW
-  TRAFFIC CONTROL DRUM WITH/WITHOUT LIGHT
-  TYPE III BARRICADE WITH/WITHOUT ATTACHED SIGN
-  TRAFFIC CONTROL SIGN ON PERMANENT/TEMPORARY SUPPORT

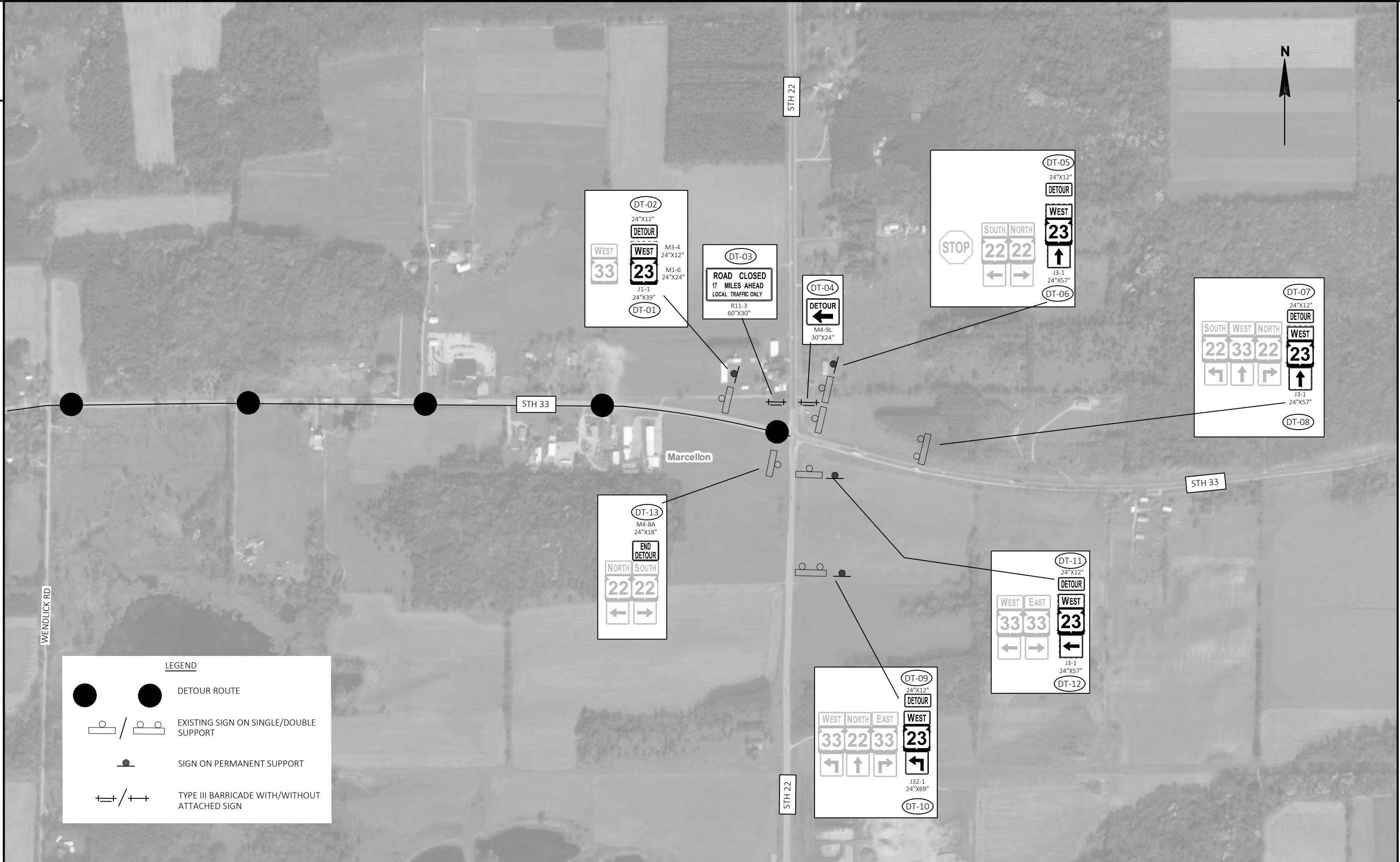


STAGE ONE CLOSURES:






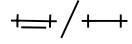

STH 22 SOUTHBOUND LANE CLOSED (SOUTHBOUND TURNING MOVEMENT RESTRICTED)
TRAFFIC DETOURED VIA I-39 TO STH 16 TO STH 33

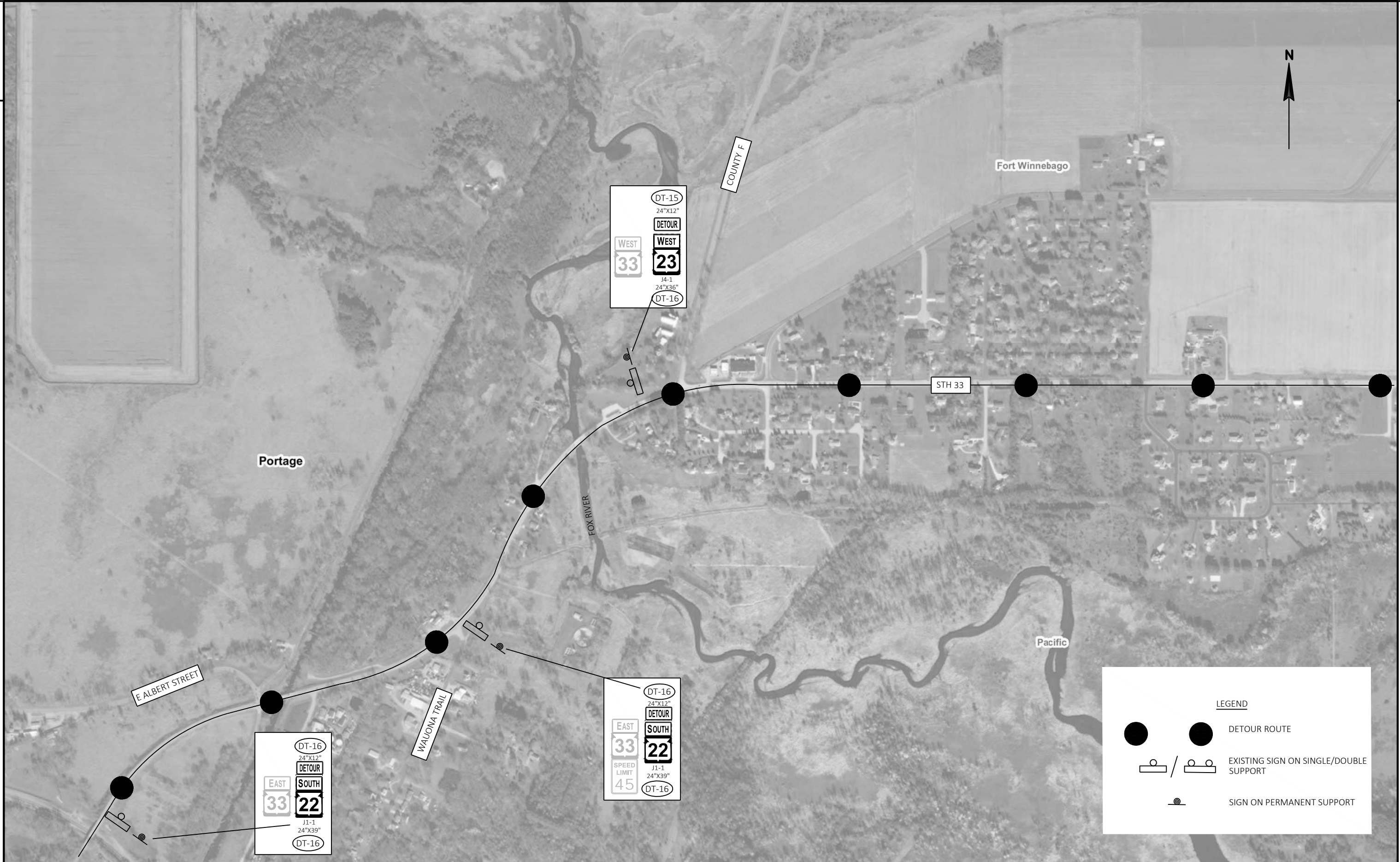
STH 23 EASTBOUND LANE CLOSED (WESTBOUND TURNING MOVEMENT RESTRICTED)
TRAFFIC DETOURED VIA STH 33 TO STH 16 TO I-39

STAGE TWO DOES NOT REQUIRE A DETOUR



LEGEND

-   DETOUR ROUTE
-  /  EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
-  SIGN ON PERMANENT SUPPORT
-  /  TYPE III BARRICADE WITH/WITHOUT ATTACHED SIGN



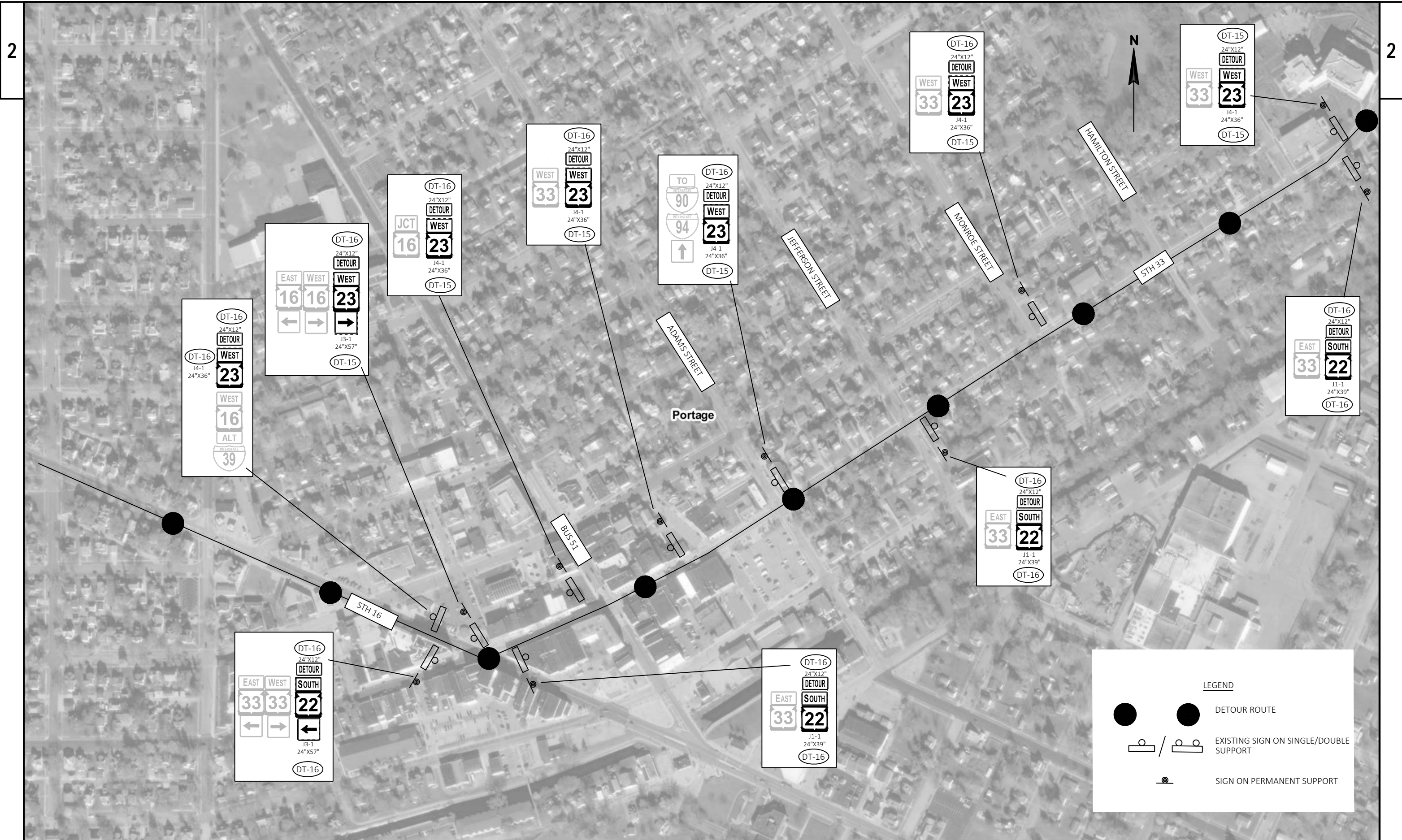
DT-15
24"x12"
DETOUR
WEST
33
WEST
23
J4-1
24"x36"
DT-16

DT-16
24"x12"
DETOUR
EAST
33
SOUTH
22
SPEED LIMIT
45
J1-1
24"x39"
DT-16

DT-16
24"x12"
DETOUR
EAST
33
SOUTH
22
J1-1
24"x39"
DT-16

LEGEND

- DETOUR ROUTE
- / ○ EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
- ⊙ SIGN ON PERMANENT SUPPORT



2

2

PROJECT NO: 6054-05-75

HWY: 22

COUNTY: MARQUETTE

DETOUR - STAGE ONE (SHEET C)

SHEET

E

FILE NAME : N:\PDS\C3D\60540505\SHEETSPLAN\027001_DT.DWG
LAYOUT NAME - 027003_s1_dt

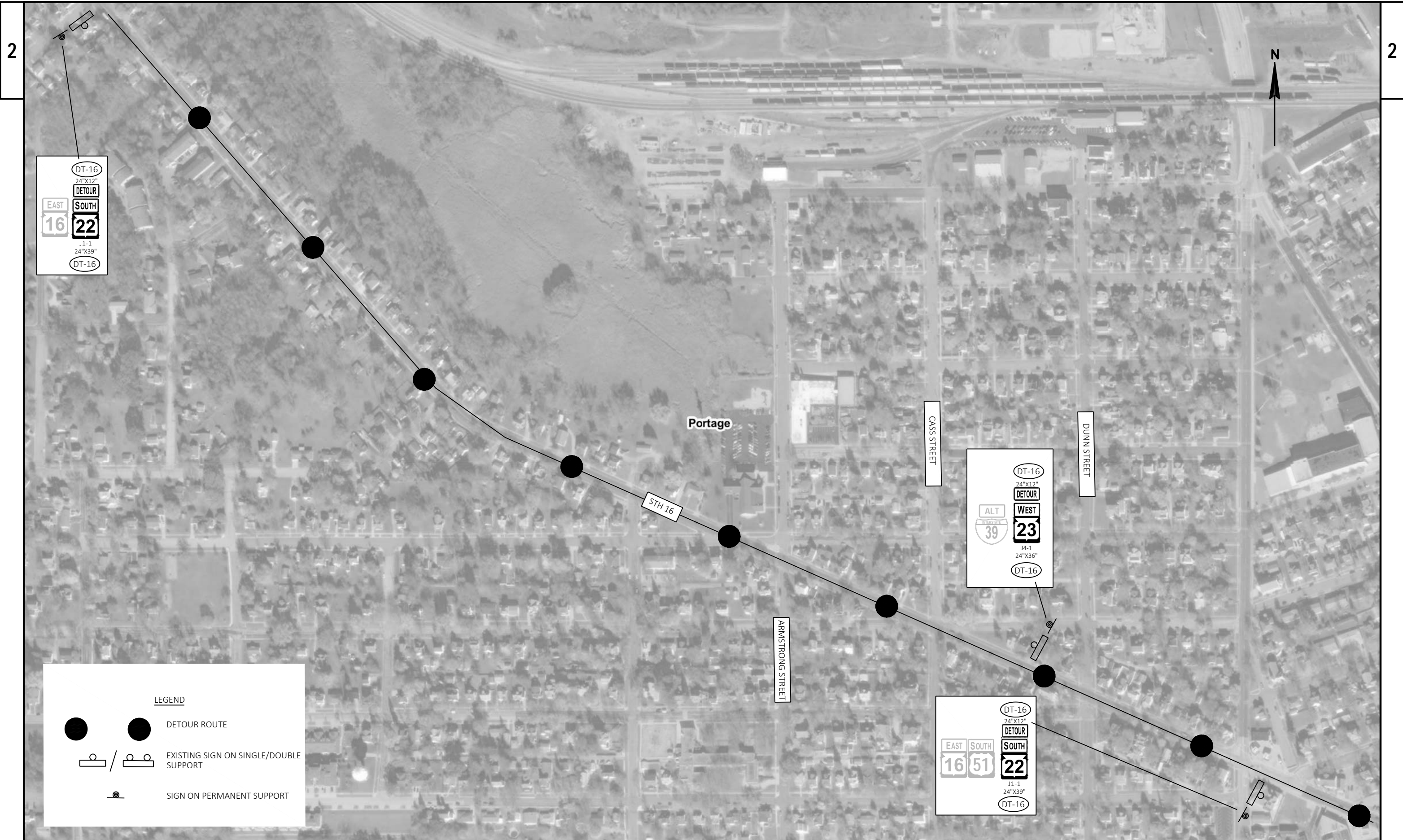
PLOT DATE : 1/25/2018 2:01 PM

PLOT BY : OLSEN, REBECCA J



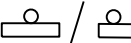


PLOT NAME :

PLOT SCALE : #####

WISDOT/CADD SHEET 42

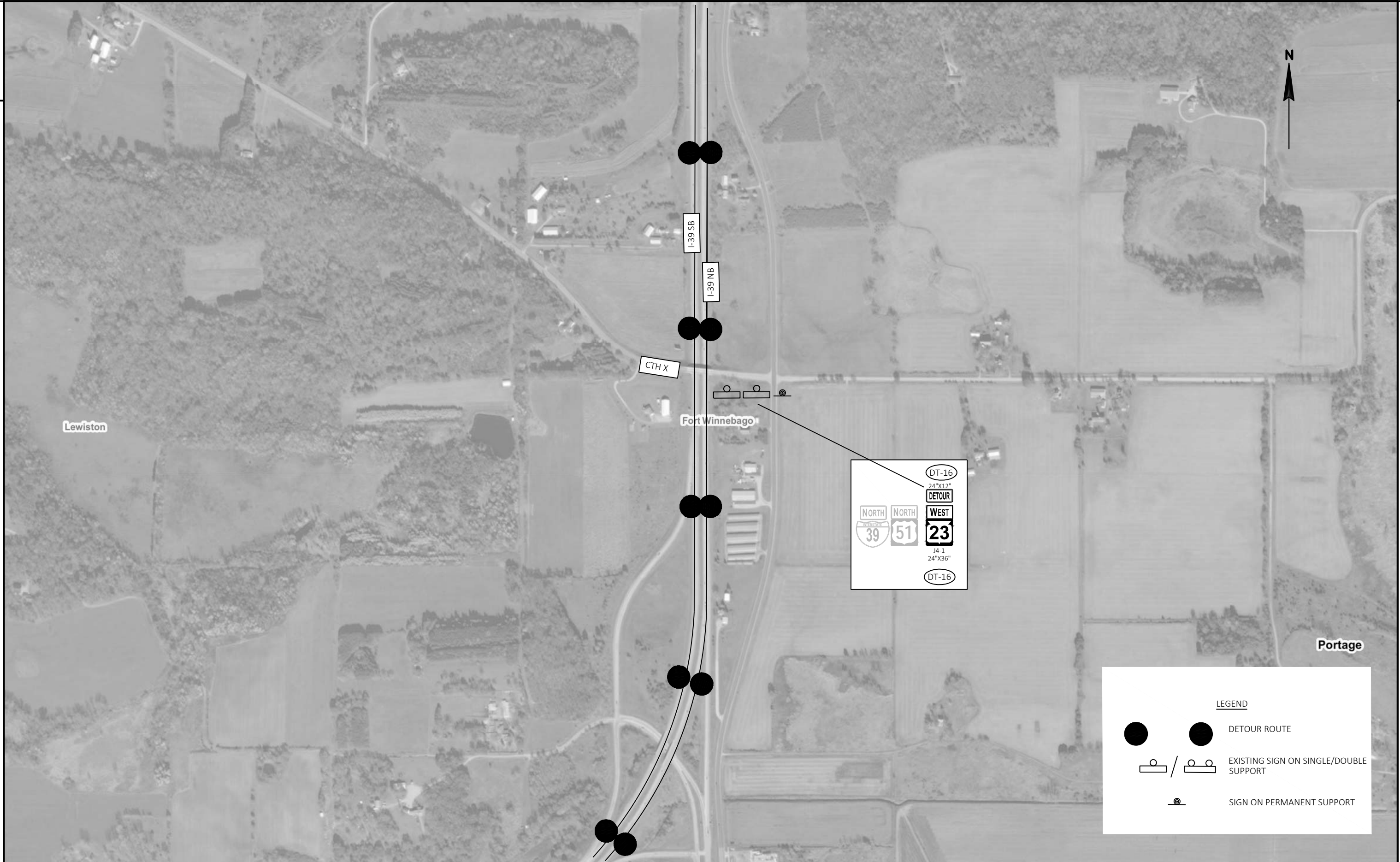


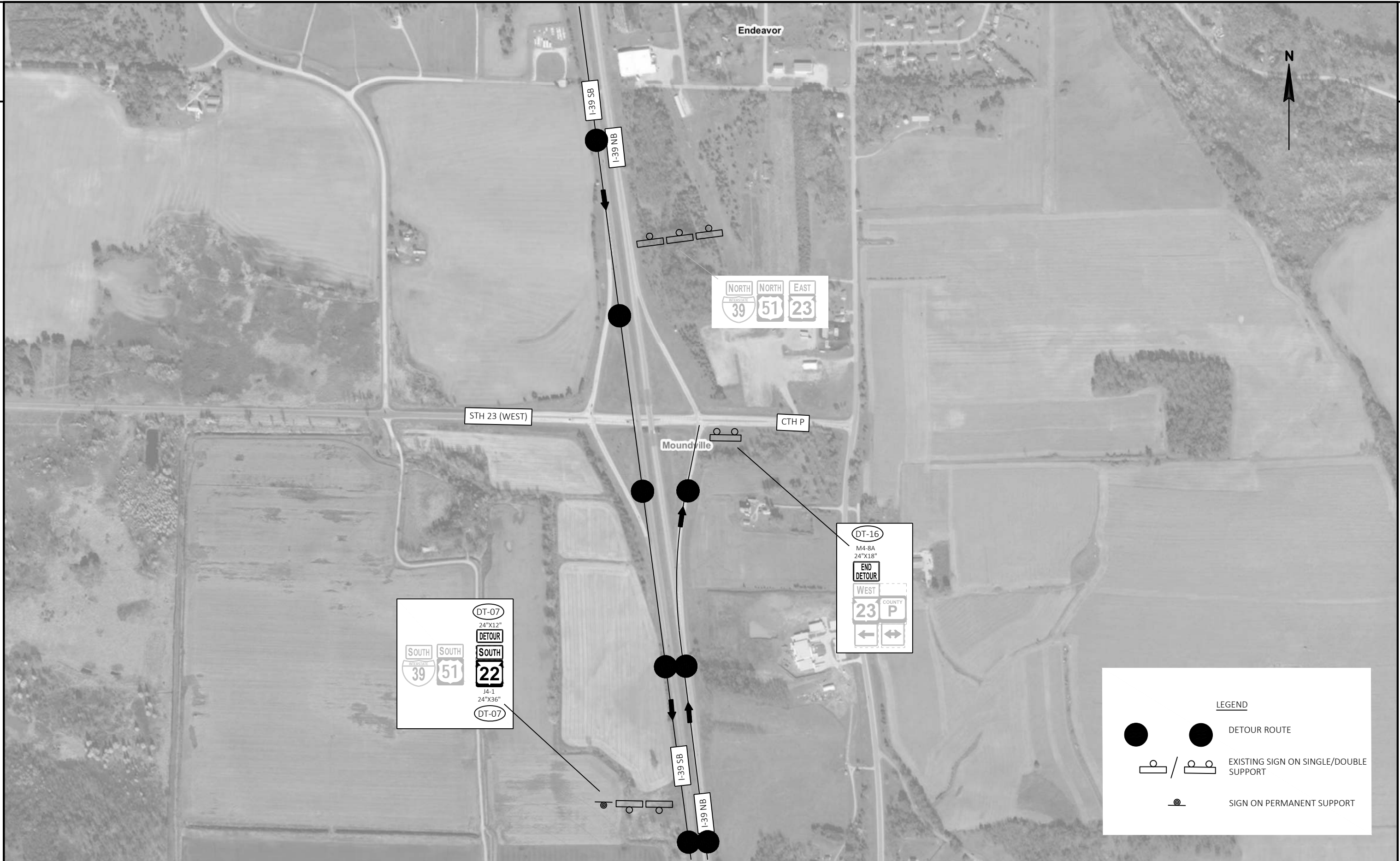
LEGEND

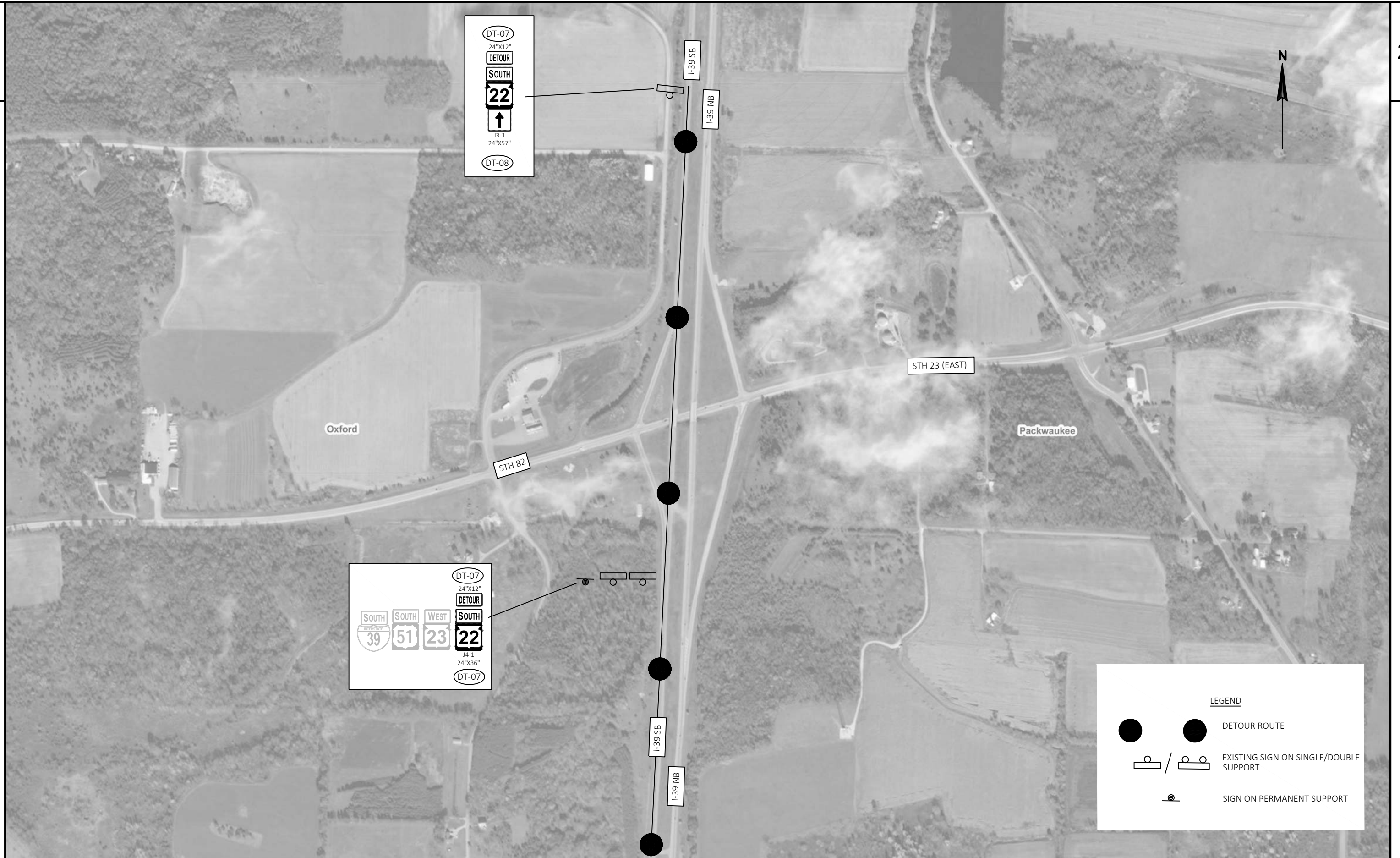
-   DETOUR ROUTE
-  /  EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
-  SIGN ON PERMANENT SUPPORT

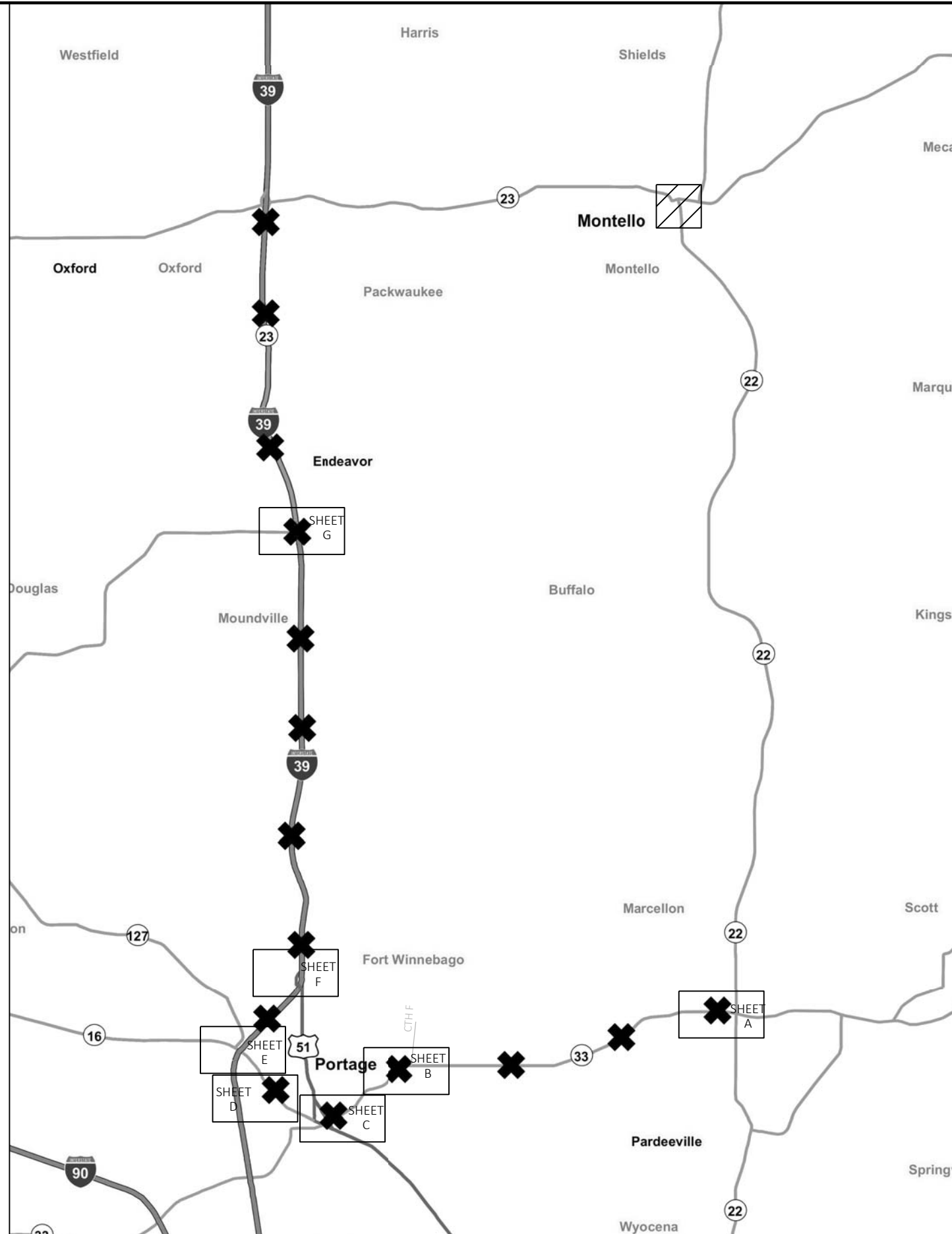
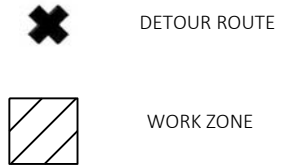
PROJECT NO: 6054-05-75 HWY: 22 COUNTY: MARQUETTE DETOUR - STAGE ONE (SHEET D) SHEET E



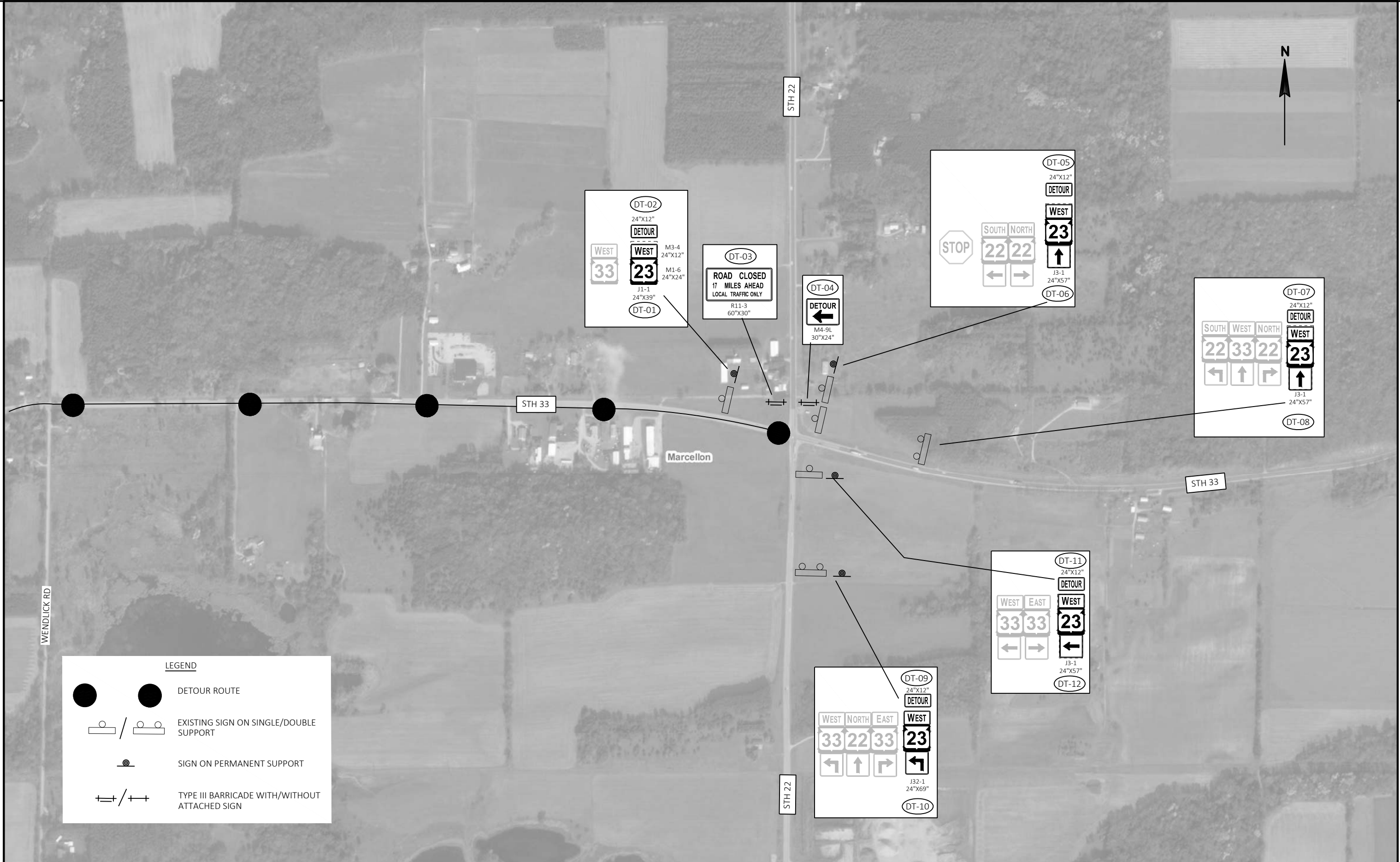









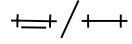



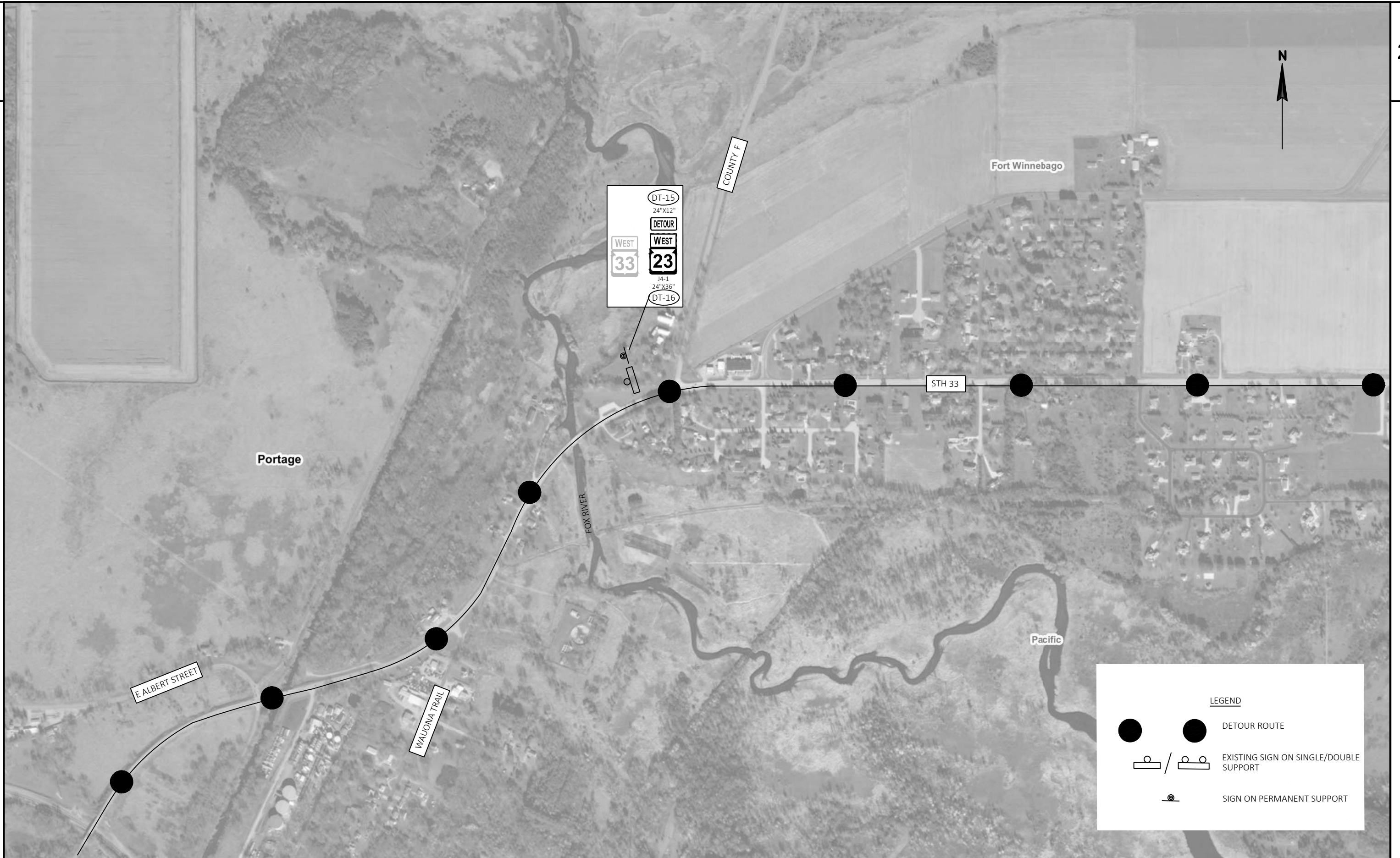


STAGE THREE CLOSURES:
 STH 23 WESTBOUND LANE CLOSED (WESTBOUND TURNING
 MOVEMENT RESTRICTED)
 TRAFFIC DETOURED VIA STH 33 TO STH 16 TO I-39



LEGEND

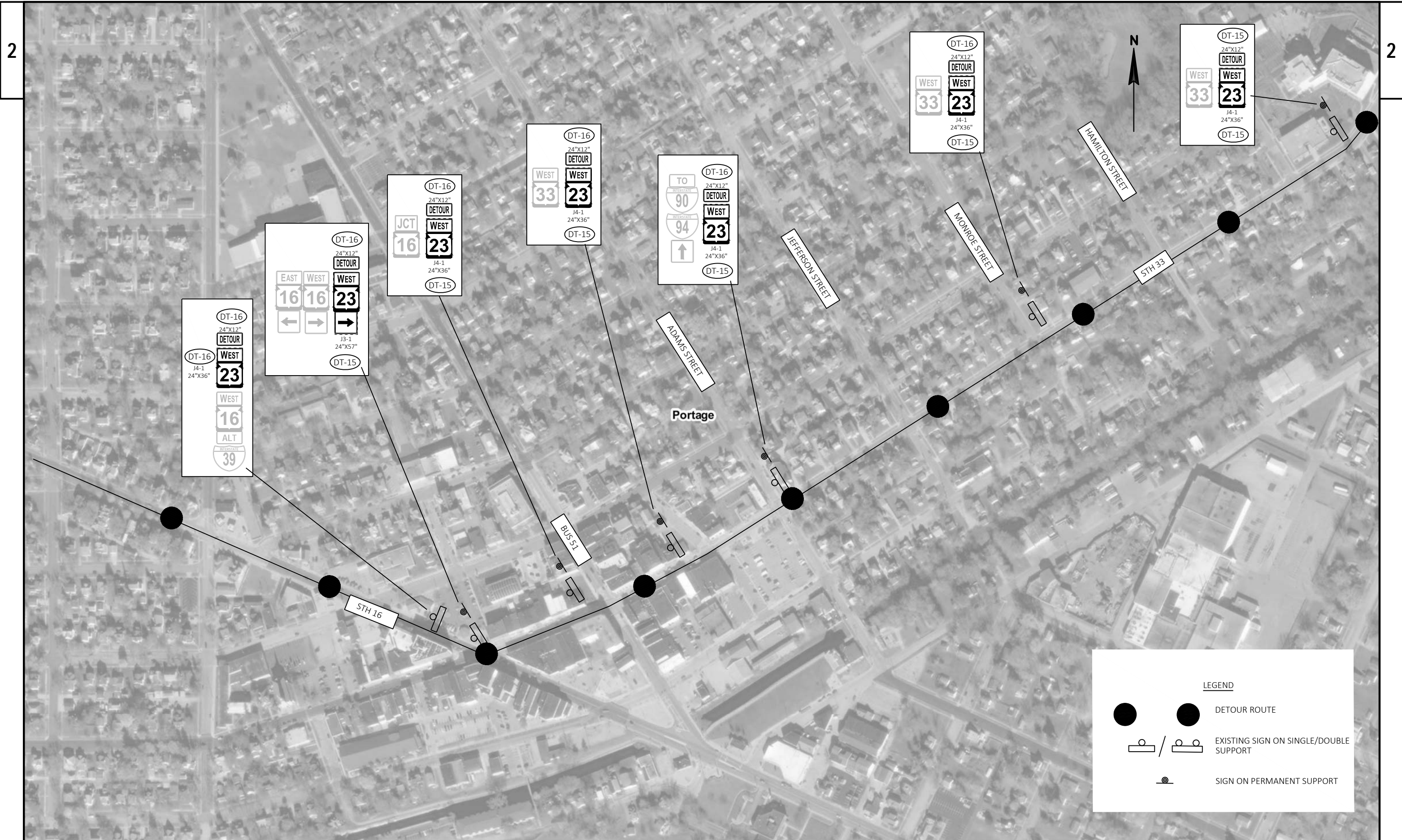
-   DETOUR ROUTE
-  /  EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
-  SIGN ON PERMANENT SUPPORT
-  /  TYPE III BARRICADE WITH/WITHOUT ATTACHED SIGN

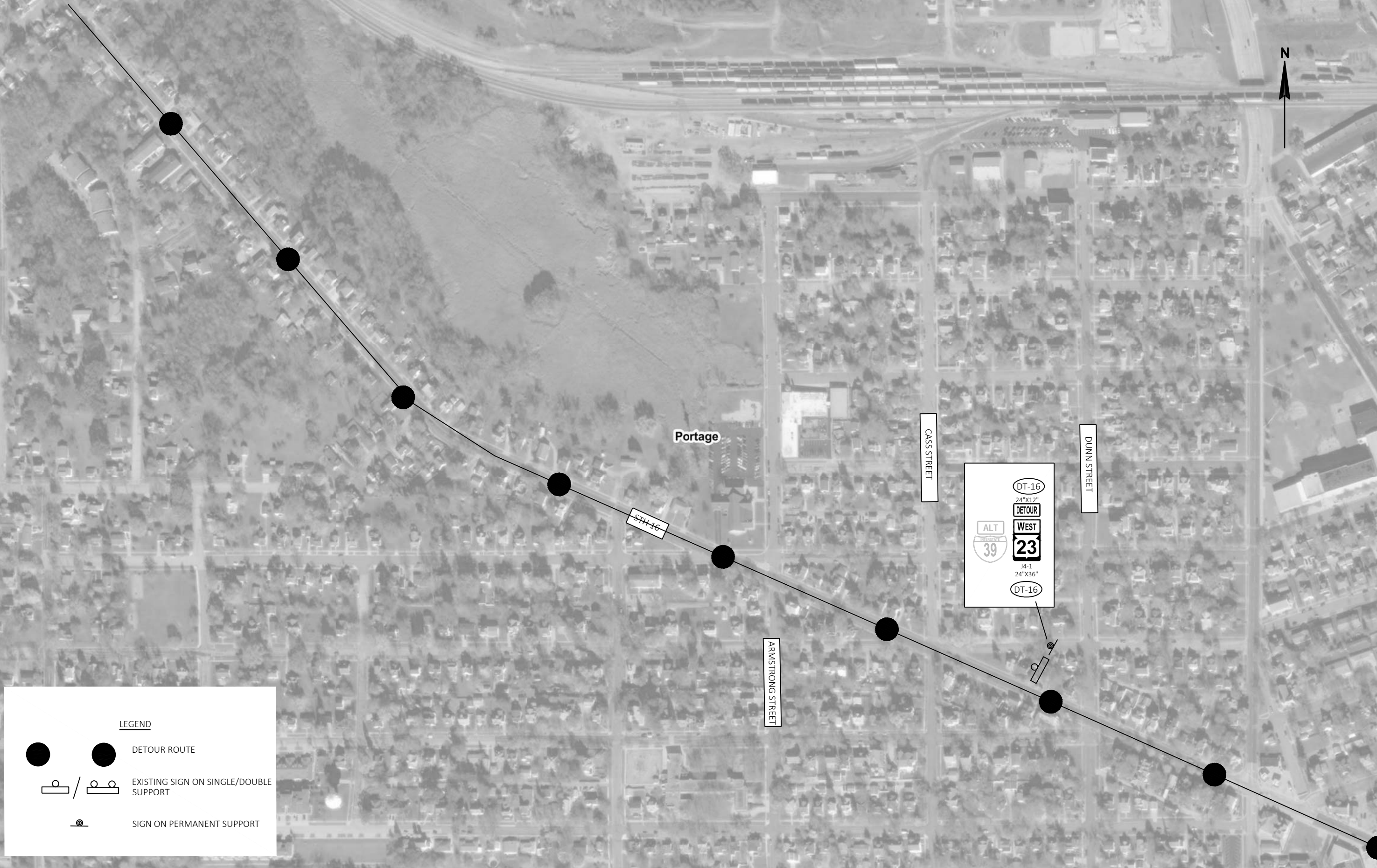


DT-15
24"x12"
DETOUR
WEST
33
WEST
23
J4-1
24"x36"
DT-16






LEGEND

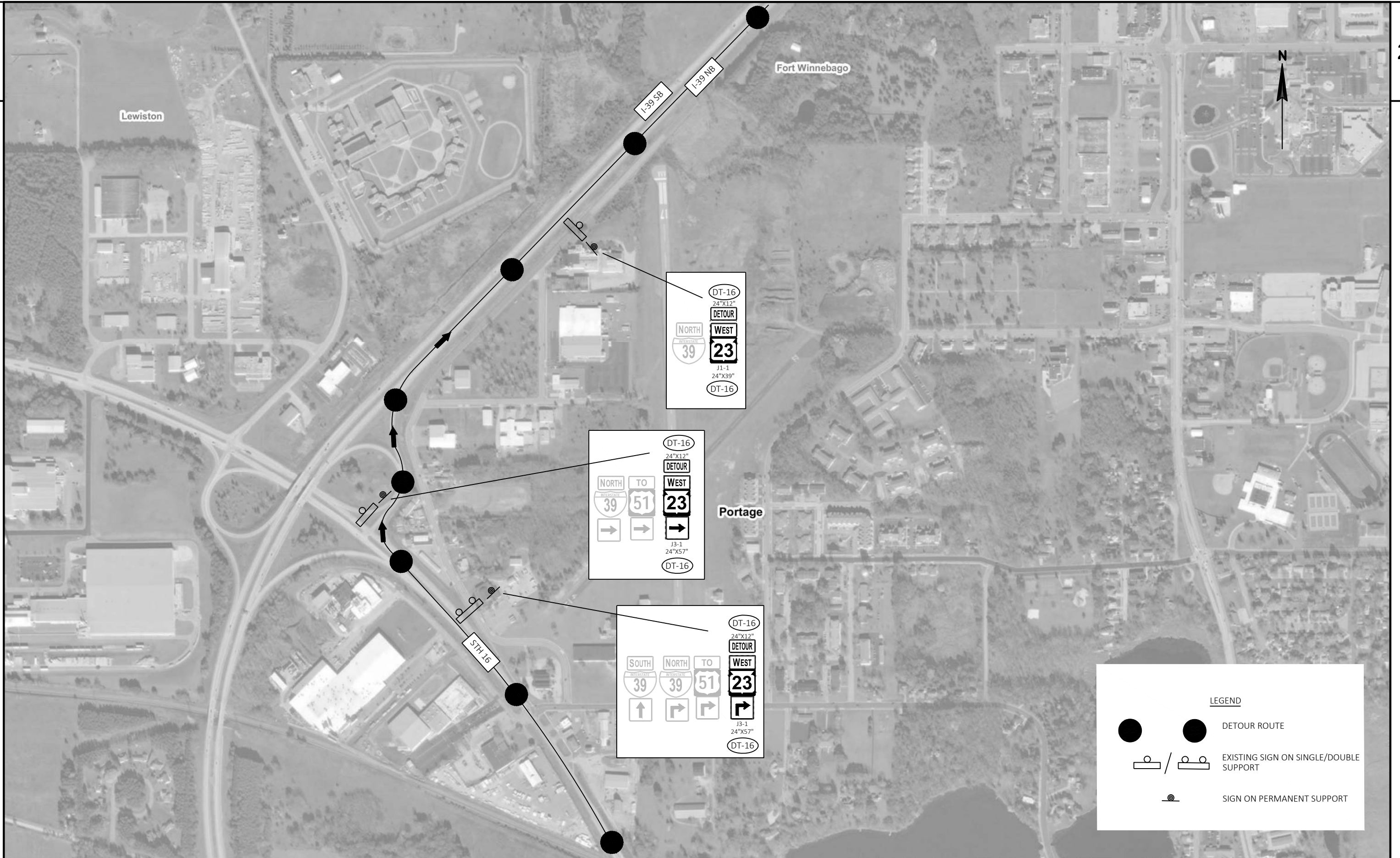
- DETOUR ROUTE
- / ○ EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
- ⊙ SIGN ON PERMANENT SUPPORT

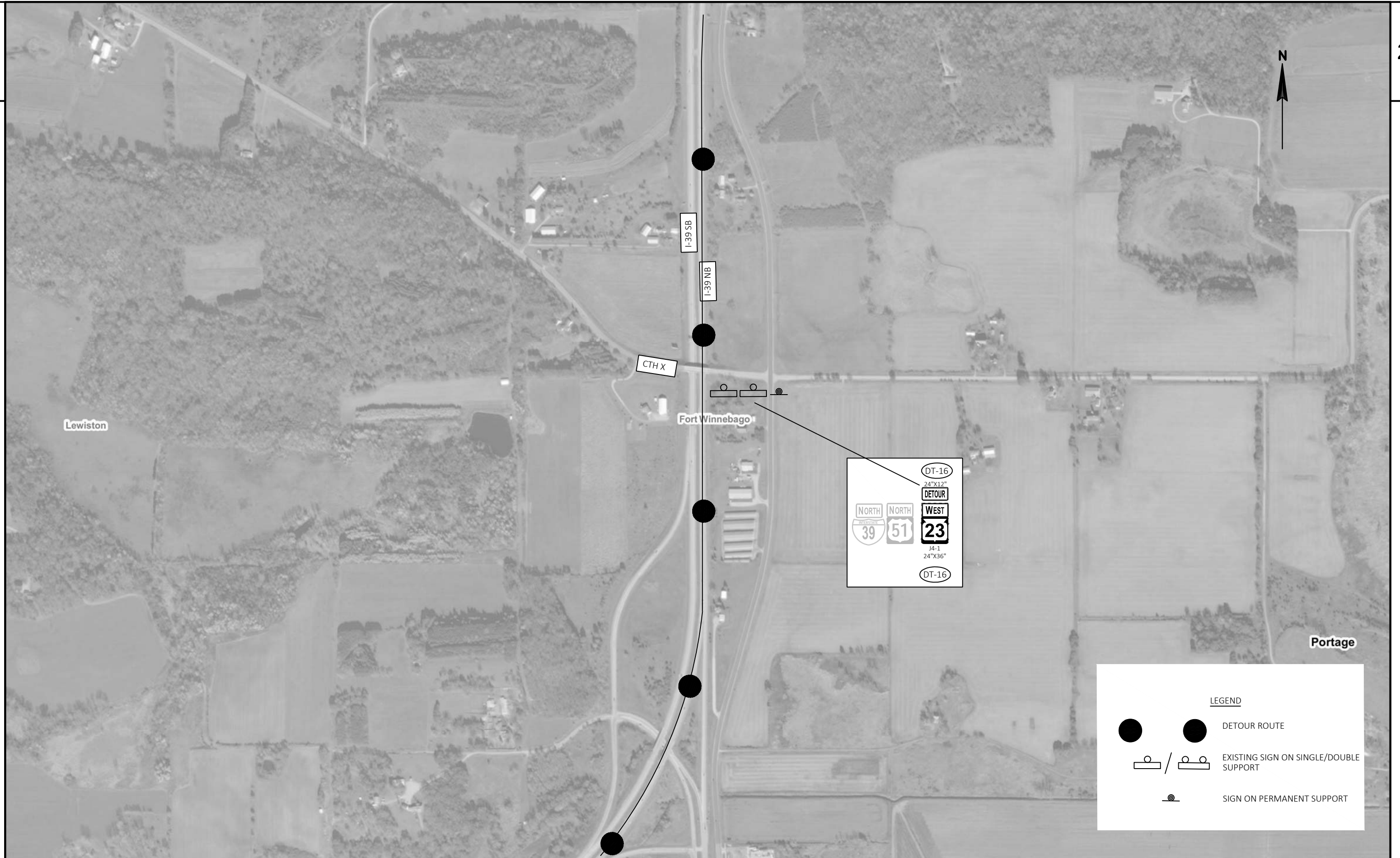




LEGEND

-   DETOUR ROUTE
-  /  EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
-  SIGN ON PERMANENT SUPPORT





DT-16
24"x12"
DETOUR

NORTH
INTERSTATE
39

NORTH
51

WEST
23

J4-1
24"x36"

DT-16

LEGEND

- DETOUR ROUTE
- / ○ EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
- ⊙ SIGN ON PERMANENT SUPPORT



DT-16
M4-8A
24"x18"

END
DETOUR

WEST

23 COUNTY P

← →

LEGEND

- DETOUR ROUTE
- / ○ EXISTING SIGN ON SINGLE/DOUBLE SUPPORT
- ⊙ SIGN ON PERMANENT SUPPORT

CONCRETE BARRIER TEMPORARY PRECAST

STATION - STATION	LOCATION	603.8000 603.8125		REMARKS
		DELIVERED LF	INSTALLED LF	
8"K"+59 - 9"K"+50	LT	100	100	TRAFFIC CONTROL STAGE ONE, STAGE TWO
8"K"+50 - 9"K"+50	RT	0	100	TRAFFIC CONTROL STAGE THREE
TOTAL:		100	200	

TRAFFIC CONTROL DRUMS

LOCATION	DAYS NEEDED	643.0300		REMARKS
		NUMBER REQUIRED	DAY	
STH 22	21	55	1155	TRAFFIC CONTROL STAGE ONE
STH 22	21	33	693	TRAFFIC CONTROL STAGE TWO
STH 22	21	20	420	TRAFFIC CONTROL STAGE THREE
STH 22	5	42	210	TRAFFIC CONTROL STAGE FOUR
STH 22	5	22	110	TRAFFIC CONTROL STAGE FIVE
TOTAL:		258		

TRAFFIC CONTROL BARRICADES

LOCATION	DAYS NEEDED	TYPE II		TYPE III		REMARKS
		NUMBER 643.0410	DAY	NUMBER 643.0420	DAY	
STH 22	21	0	0	2	42	DETOUR - STAGE ONE
STH 22	21	24	504	32	672	TRAFFIC CONTROL STAGE ONE
STH 22	21	22	462	26	546	TRAFFIC CONTROL STAGE TWO
STH 22	21	18	378	22	462	TRAFFIC CONTROL STAGE THREE
STH 22	5	0	0	4	20	TRAFFIC CONTROL STAGE FOUR
STH 22	5	0	0	2	10	TRAFFIC CONTROL STAGE FIVE
STH 22	21	0	0	2	42	DETOUR - STAGE THREE
TOTAL:		1344		1794		

TRAFFIC CONTROL WARNING LIGHTS

LOCATION	DAYS NEEDED	TYPE A		TYPE C		REMARKS
		NUMBER 643.0705	DAY	NUMBER 643.0715	DAY	
STH 22	21	2	42	0	0	DETOUR - STAGE ONE
STH 22	21	10	210	45	945	TRAFFIC CONTROL STAGE ONE
STH 22	21	11	231	22	462	TRAFFIC CONTROL STAGE TWO
STH 22	21	9	189	11	231	TRAFFIC CONTROL STAGE THREE
STH 22	5	10	50	32	160	TRAFFIC CONTROL STAGE FOUR
STH 22	5	10	50	12	60	TRAFFIC CONTROL STAGE FIVE
STH 22	21	2	42	0	0	DETOUR - STAGE THREE
TOTAL:		814		1858		

TRAFFIC CONTROL SIGNS

SIGN NO.	SIGN CODE	MESSAGE	SIZE	TYPE II		643.0900	REMARKS
				REFLECTIVE H	REFLECTIVE F	TOTAL DAYS	
TC01	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC02	G20-2A	END ROAD WORK	48"X24"	---	1	80	ENTIRE PROJECT DURATION
TC03	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC04	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC05	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC06	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC07	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC08	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC09	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC10	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC11	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC12	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC13	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC14	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC15	G20-2A	END ROAD WORK	48"X24"	---	1	80	ENTIRE PROJECT DURATION
TC16	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC17	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC18	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC19	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC20	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC21	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC22	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC23	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC24	G20-2A	END ROAD WORK	48"X24"	---	1	80	ENTIRE PROJECT DURATION
TC25	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	80	ENTIRE PROJECT DURATION
TC26	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	80	ENTIRE PROJECT DURATION
S101	W20-4A	ONE LANE ROAD AHEAD	48"X48"	---	1	21	STAGE ONE
S103	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE ONE
S104	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE ONE
S105	R9-8	PEDESTRIAN CROSSWALK	24"X12"	1	---	21	STAGE ONE
S106	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S107	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S108	R3-50-R	LANE CONTROL SYMBOL - RIGHT ONLY	30"X36"	1	---	21	STAGE ONE
S109	R3-50-R	LANE CONTROL SYMBOL - RIGHT ONLY	30"X36"	1	---	21	STAGE ONE
S110	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE ONE
S111	W01-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE ONE
S112	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S113	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S114	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S115	R4-7	KEEP RIGHT	24"X30"	1	---	21	STAGE ONE
S116	R3-2	NO LEFT TURN SYMBOL	24"X24"	1	---	21	STAGE ONE
S117	R3-53-R	RIGHT TURN ONLY	24"X30"	1	---	21	STAGE ONE
S118	R3-53-R	RIGHT TURN ONLY	24"X30"	1	---	21	STAGE ONE
S119	R4-7	KEEP RIGHT	24"X30"	1	---	21	STAGE ONE
S120	R3-2	NO LEFT TURN SYMBOL	24"X24"	1	---	21	STAGE ONE
S121	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE ONE
S122	W01-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE ONE
S123	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S124	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S125	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S127	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE

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TRAFFIC CONTROL SIGNS

SIGN NO.	SIGN CODE	MESSAGE	SIZE	643.0900		TOTAL DAYS	REMARKS
				TYPE II REFLECTIVE H	TYPE II REFLECTIVE F		
S128	R7-1-R	NO PARKING ANYTIME [RIGHT ARROW]	18"X24"	1	---	21	STAGE ONE
S129	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE ONE
S130	WO1-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE ONE
S131	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S132	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE ONE
S133	R7-1-R	NO PARKING ANYTIME [RIGHT ARROW]	18"X24"	1	---	21	STAGE ONE
S134	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S135	R7-1-L	NO PARKING ANYTIME [LEFT ARROW]	18"X24"	1	---	21	STAGE ONE
S136	W20-4A	ONE LANE ROAD AHEAD	48"X48"	---	1	21	STAGE ONE
S137	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE ONE
S138	R7-1-R	NO PARKING ANYTIME [RIGHT ARROW]	18"X24"	1	---	21	STAGE ONE
S139	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S140	R7-1-L	NO PARKING ANYTIME [LEFT ARROW]	18"X24"	1	---	21	STAGE ONE
S141	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S142	W1-4-R	RIGHT REVERSE CURVE	30"X30"	---	1	21	STAGE ONE
S145	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE ONE
S146	J4-2	SOUTH, 22, WEST, 23	48"X36"	1	---	21	STAGE ONE
S146	R4-7	KEEP RIGHT	24"X30"	1	---	21	STAGE ONE
S146	R4-8	KEEP LEFT	24"X30"	1	---	21	STAGE ONE
S147	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE ONE
S148	R3-8E	LEFT ONLY / AHEAD ONLY	36"X30"	1	---	21	STAGE ONE
S149	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S150	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S151	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S152	W20-4A	ONE LANE ROAD AHEAD	48"X48"	---	1	21	STAGE ONE
S153	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S154	R9-8	PEDESTRIAN CROSSWALK	24"X12"	1	---	21	STAGE ONE
S155	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE ONE
S156	R9-8	PEDESTRIAN CROSSWALK	24"X12"	1	---	21	STAGE ONE
S157	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S158	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE ONE
S201	W20-4A	ONE LANE ROAD AHEAD	48"X48"	---	1	21	STAGE TWO
S202	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE TWO
S203	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE TWO
S204	R9-8	PEDESTRIAN CROSSWALK	24"X12"	1	---	21	STAGE TWO
S205	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S206	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S207	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE TWO
S208	WO1-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE TWO
S209	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S210	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S211	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S212	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S213	WO1-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE TWO
S214	R3-2	NO LEFT TURN SYMBOL	24"X24"	1	---	21	STAGE TWO
S215	R3-53-R	RIGHT TURN ONLY	24"X30"	1	---	21	STAGE TWO
S216	R3-53-R	RIGHT TURN ONLY	24"X30"	1	---	21	STAGE TWO
S217	R3-2	NO LEFT TURN SYMBOL	24"X24"	1	---	21	STAGE TWO
S218	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S219	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE TWO
S220	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S221	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S222	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S223	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE TWO
S224	WO1-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE TWO
S225	R7-1-D	NO PARKING ANYTIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO

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TRAFFIC CONTROL SIGNS

SIGN NO.	SIGN CODE	MESSAGE	SIZE	643.0900		TOTAL DAYS	REMARKS
				TYPE II REFLECTIVE H	TYPE II REFLECTIVE F		
S226	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE TWO
S227	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S228	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S229	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S230	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE TWO
S231	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S232	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S233	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S234	R9-8	PEDESTRIAN CROSSWALK	24"X12"	1	---	21	STAGE TWO
S235	WO1-7	NIGHT ARROW - DOUBLE	48"X24"	---	1	21	STAGE TWO
S236	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE TWO
S237	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE TWO
S238	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S239	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S240	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S241	W20-4A	ONE LANE ROAD AHEAD	48"X48"	---	1	21	STAGE TWO
S242	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S243	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S244	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE TWO
S301	W20-4A	ONE LANE ROAD AHEAD	48"X48"	---	1	21	STAGE THREE
S303	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE THREE
S304	R3-50-R	LANE CONTROL SYMBOL - RIGHT ONLY	30"X36"	1	---	21	STAGE THREE
S305	R3-50-R	LANE CONTROL SYMBOL - RIGHT ONLY	30"X36"	1	---	21	STAGE THREE
S306	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE THREE
S307	WO1-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE THREE
S308	R3-2	NO LEFT TURN SYMBOL	24"X24"	1	---	21	STAGE THREE
S309	R3-53-R	RIGHT TURN ONLY	24"X30"	1	---	21	STAGE THREE
S310	R4-7	KEEP RIGHT	24"X30"	1	---	21	STAGE THREE
S312	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S313	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S314	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE THREE
S315	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S316	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S317	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S318	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE THREE
S319	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S320	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S321	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S322	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE THREE
S323	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE THREE
S324	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE THREE
S325	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE THREE
S328	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE THREE
S329	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE THREE
S330	R9-9	SIDEWALK CLOSED	24"X12"	1	---	21	STAGE THREE
S331	R4-7	KEEP RIGHT	24"X30"	1	---	21	STAGE THREE
S332	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE THREE
S333	WO1-6	NIGHT ARROW - SINGLE	48"X24"	---	1	21	STAGE THREE
S334	R10-6	STOP HERE ON RED	24"X36"	---	---	21	STAGE THREE

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TRAFFIC CONTROL SIGNS

SIGN NO.	SIGN CODE	MESSAGE	SIZE	643.0900		TOTAL DAYS	REMARKS
				TYPE II REFLECTIVE H	TYPE II REFLECTIVE F		
S335	W1-4-L	LEFT REVERSE CURVE	30"X30"	---	1	21	STAGE THREE
S336	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S337	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S338	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S339	W20-4A	ONE LANE ROAD AHEAD	48"X48"	---	1	21	STAGE THREE
S340	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S341	R9-8	PEDESTRIAN CROSSWALK	24"X12"	1	---	21	STAGE THREE
S342	R9-11	SIDEWALK CLOSED AHEAD CROSS HERE	24"X12"	1	---	21	STAGE THREE
S343	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S344	R7-1-D	NO PARKING ANY TIME [DOUBLE ARROW]	18"X24"	1	---	21	STAGE THREE
S401	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE FOUR
S402	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE FOUR
S501	R11-2L	LANE CLOSED	48"X30"	1	---	21	STAGE FIVE
D101	J3-1	WEST, 23, [UP ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D102	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D103	J3-1	WEST, 23, [UP ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D104	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D105	J3-1	WEST, 23, [LEFT ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D106	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D107	J2-1	WEST, 23, [AHEAD LEFT ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D108	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D109	M4-9L	DETOUR SIGN WITH LEFT ARROW	30"X24"	---	1	21	DETOUR - STAGE ONE
D110	M4-8A	END DETOUR	24"X18"	---	1	21	DETOUR - STAGE ONE
		ROAD CLOSED 17 MILES AHEAD					
D111	R11-3	LOCAL TRAFFIC ONLY	60"X30"	1	---	21	DETOUR - STAGE ONE
D112	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE ONE
D119	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D120	J4-1	SOUTH, 22	24"X36"	1	---	21	DETOUR - STAGE ONE
D121	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D122	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE ONE
D123	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D124	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE ONE
D125	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D126	J4-1	SOUTH, 22	24"X36"	1	---	21	DETOUR - STAGE ONE
D127	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D128	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE ONE
D129	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D130	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE ONE
D131	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D132	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE ONE
D133	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D134	J4-1	SOUTH, 22	24"X36"	1	---	21	DETOUR - STAGE ONE
D135	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D136	J3-1	WEST, 23, [RIGHT ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D137	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D138	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE ONE
D139	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D140	J3-1	SOUTH, 22, [LEFT ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D141	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE

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TRAFFIC CONTROL SIGNS

SIGN NO.	SIGN CODE	MESSAGE	SIZE	643.0900		TOTAL DAYS	REMARKS
				TYPE II REFLECTIVE H	TYPE II REFLECTIVE F		
D148	J4-1	SOUTH, 22	24"X36"	1	---	21	DETOUR - STAGE ONE
D149	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D150	J2-1	WEST, 23, [AHEAD RIGHT ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D151	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D152	J4-1	SOUTH, 22	24"X36"	1	---	21	DETOUR - STAGE ONE
D153	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D154	J3-1	WEST, 23, [RIGHT ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D155	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D156	J3-1	SOUTH, 22, [ARROW TILT RIGHT]	24"X57"	1	---	21	DETOUR - STAGE ONE
D157	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D158	J3-1	SOUTH, 22, [ARROW TILT RIGHT]	36"X84"	1	---	21	DETOUR - STAGE ONE
D159	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE ONE
D160	J4-1	WEST, 23	36"X54"	1	---	21	DETOUR - STAGE ONE
D161	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE ONE
D162	J4-1	WEST, 23	36"X54"	1	---	21	DETOUR - STAGE ONE
D163	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE ONE
D164	J4-1	SOUTH, 22	36"X54"	1	---	21	DETOUR - STAGE ONE
D165	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE ONE
D166	M4-8A	END DETOUR	24"X18"	---	1	21	DETOUR - STAGE ONE
D167	J4-1	SOUTH, 22	36"X54"	1	---	21	DETOUR - STAGE ONE
D168	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE ONE
D169	J3-1	SOUTH, 22, [RIGHT ARROW]	24"X57"	1	---	21	DETOUR - STAGE ONE
D170	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE ONE
D171	J3-1	SOUTH, 22, [UP ARROW]	36"X84"	1	---	21	DETOUR - STAGE ONE
D172	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE ONE
D301	J3-1	WEST, 23, [UP ARROW]	24"X57"	1	---	21	DETOUR - STAGE THREE
D302	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D303	J3-1	WEST, 23, [UP ARROW]	24"X57"	1	---	21	DETOUR - STAGE THREE
D304	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D305	J3-1	WEST, 23, [LEFT ARROW]	24"X57"	1	---	21	DETOUR - STAGE THREE
D306	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D307	J2-1	WEST, 23, [AHEAD LEFT ARROW]	24"X57"	1	---	21	DETOUR - STAGE THREE
D308	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D309	M4-9L	DETOUR SIGN WITH LEFT ARROW ROAD CLOSED 17 MILES AHEAD	30"X24"	---	1	21	DETOUR - STAGE THREE
D310	R11-3	LOCAL TRAFFIC ONLY	60"X30"	1	---	21	DETOUR - STAGE THREE
D311	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D312	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D313	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D314	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D315	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D316	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D317	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D318	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D319	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D320	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D321	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D322	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE

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TRAFFIC CONTROL SIGNS

SIGN NO.	SIGN CODE	MESSAGE	SIZE	643.0900		TOTAL DAYS	REMARKS
				TYPE II REFLECTIVE H	TYPE II REFLECTIVE F		
D323	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D324	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D325	J3-1	WEST, 23, [RIGHT ARROW]	24"X57"	1	---	21	DETOUR - STAGE THREE
D326	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D327	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D328	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D329	J4-1	WEST, 23	24"X36"	1	---	21	DETOUR - STAGE THREE
D330	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D331	J2-1	WEST, 23, [A HEAD RIGHT ARROW]	24"X57"	1	---	21	DETOUR - STAGE THREE
D332	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D333	J3-1	WEST, 23, [RIGHT ARROW]	24"X57"	1	---	21	DETOUR - STAGE THREE
D334	M4-8	DETOUR	24"X12"	---	1	21	DETOUR - STAGE THREE
D335	J4-1	WEST, 23	36"X54"	1	---	21	DETOUR - STAGE THREE
D336	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE THREE
D337	J4-1	WEST, 23	36"X54"	1	---	21	DETOUR - STAGE THREE
D338	M4-8	DETOUR	36"X18"	---	1	21	DETOUR - STAGE THREE
D339	M4-8A	END DETOUR	24"X18"	---	1	21	DETOUR - STAGE THREE
TOTAL:						7141	

TRAFFIC CONTROL COVERING SIGNS

SIGN NO.	MESSAGE TO COVER	NUMBER OF SIGNS	NUMBER OF CYCLES	643.0920 TYPE II EACH	REMARKS
S102, S302	WEST, 23, [AHEAD LEFT ARROW]	1	2	2	INSTALL IN STAGE ONE, REMOVE PRIOR TO STAGE TWO; INSTALL IN STAGE THREE
S126	SOUTH, 22, [RIGHT ARROW]	1	1	1	INSTALL IN STAGE ONE, REMOVE PRIOR TO STAGE TWO
S143, S325	WEST, 23, [LEFT ARROW]	1	2	2	INSTALL IN STAGE ONE, REMOVE PRIOR TO STAGE TWO; INSTALL IN STAGE THREE
S144	SOUTH, 22, [LEFT ARROW]	1	1	1	INSTALL IN STAGE ONE, REMOVE PRIOR TO STAGE TWO
PROJECT TOTAL:		6			

TEMPORARY TRAFFIC SIGNALS FOR INTERSECTION

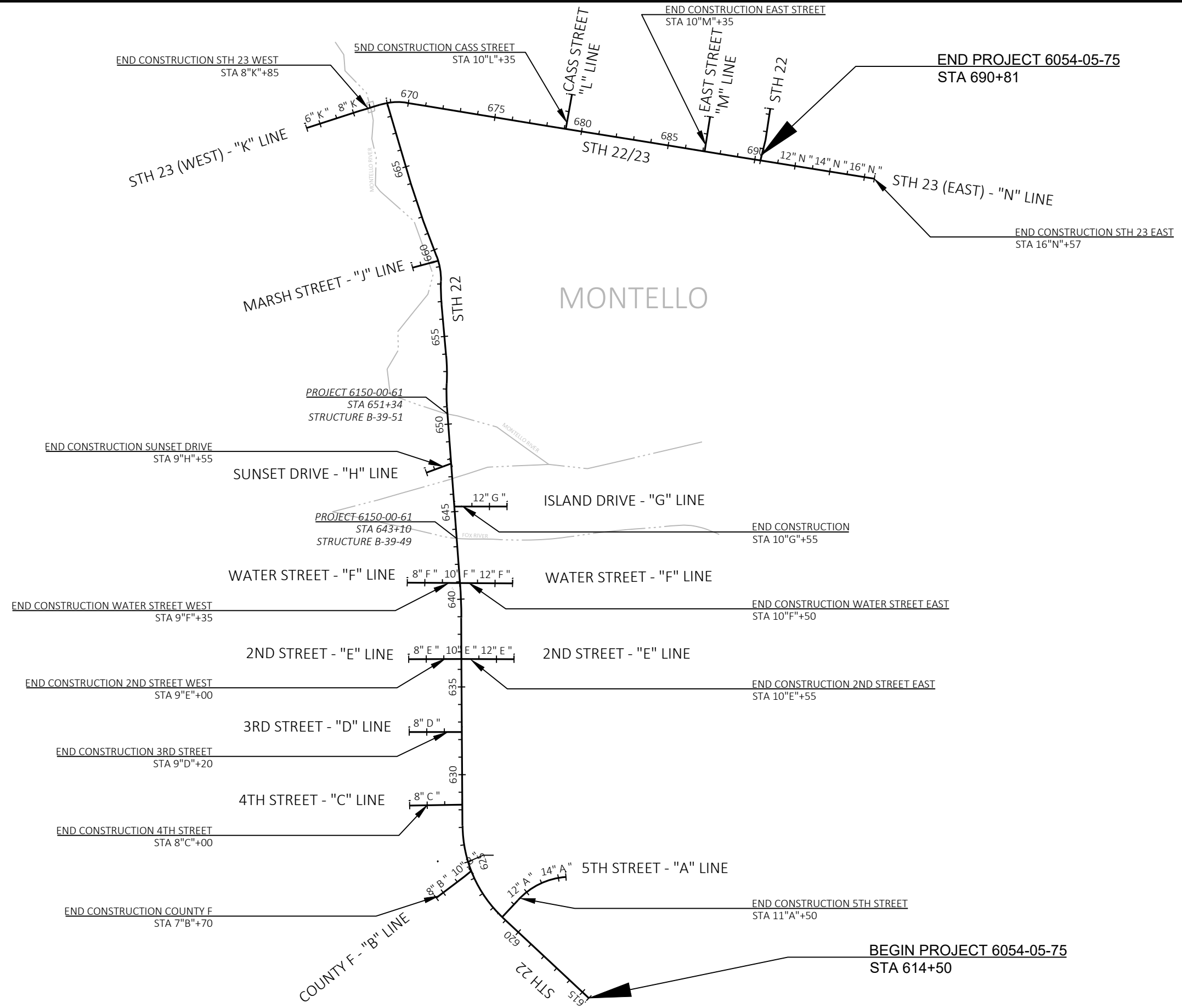
PROJECT	SPV.0105.01 LS
6054-05-75	1
TOTAL:	1

TRAFFIC CONTROL

LOCATION	646.9000 MARKING REMOVAL LINE 4-INCH LF	646.9300 MARKING REMOVAL SPECIAL MARKING EACH	649.0150 TEMPORARY MARKING LINE 4-INCH LF	649.0250 TEMPORARY MARKING LINE 8-INCH LF	649.0550 TEMPORARY MARKING ARROW TAPE EACH	649.0650 TEMPORARY MARKING WORD REMOVABLE TAPE EACH	649.0850 TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18-INCH LF	REMARKS
STH 22	500	2	230	290	4	2	66	TRAFFIC CONTROL STAGE ONE
STH 22	---	---	200	---	---	---	22	TRAFFIC CONTROL STAGE TWO
STH 22	---	---	350	---	---	---	---	TRAFFIC CONTROL STAGE THREE
STH 22	---	---	100	---	---	---	---	TRAFFIC CONTROL STAGE FOUR
STH 22	---	---	100	---	---	---	---	TRAFFIC CONTROL STAGE FIVE
TOTAL:	500	2	980	290	4	2	88	

TEMPORARY VEHICLE DETECTION STH 22/23 INTERSECTION

PROJECT	SPV.0105.02 LS
6054-05-75	1
TOTAL:	1



Estimate Of Quantities By Plan Sets

6054-05-75

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Pavement	SY	1,328.000	1,328.000
0004	204.0109.S	Removing Concrete Surface Partial Depth	SF	3,278.000	3,278.000
0006	204.0110	Removing Asphaltic Surface	SY	48.000	48.000
0008	204.0120	Removing Asphaltic Surface Milling	SY	36,616.000	36,616.000
0010	204.0150	Removing Curb & Gutter	LF	942.000	942.000
0012	204.0155	Removing Concrete Sidewalk	SY	532.000	532.000
0014	213.0100	Finishing Roadway (project) 01. 6054-05-75	EACH	1.000	1.000
0020	305.0115	Base Aggregate Dense 3/4-Inch	CY	25.000	25.000
0022	305.0125	Base Aggregate Dense 1 1/4-Inch	CY	12.000	12.000
0024	305.0500	Shaping Shoulders	STA	16.000	16.000
0026	390.0303	Base Patching Concrete	SY	500.000	500.000
0028	405.1000	Stamping Colored Concrete	CY	4.000	4.000
0030	415.0080	Concrete Pavement 8-Inch	SY	1,322.000	1,322.000
0032	416.0610	Drilled Tie Bars	EACH	166.000	166.000
0034	416.0620	Drilled Dowel Bars	EACH	570.000	570.000
0036	440.4410	Incentive IRI Ride	DOL	2,553.000	2,553.000
0038	455.0605	Tack Coat	GAL	2,556.000	2,556.000
0040	460.2000	Incentive Density HMA Pavement	DOL	2,180.000	2,180.000
0042	460.4110.S	Reheating HMA Pavement Longitudinal Joints	LF	6,501.000	6,501.000
0044	460.5224	HMA Pavement 4 LT 58-28 S	TON	3,665.000	3,665.000
0046	465.0105	Asphaltic Surface	TON	1.000	1.000
0048	509.0301	Preparation Decks Type 1	SY	15.000	15.000
0050	509.0302	Preparation Decks Type 2	SY	5.000	5.000
0052	509.0500	Cleaning Decks	SY	150.000	150.000
0054	509.1500	Concrete Surface Repair	SF	5.000	5.000
0056	509.2000	Full-Depth Deck Repair	SY	1.000	1.000
0058	509.2500	Concrete Masonry Overlay Decks	CY	9.000	9.000
0060	509.5100.S	Polymer Overlay	SY	150.000	150.000
0062	517.1800.S	Structure Repainting Recycled Abrasive (structure) 01. B-39-970	LS	1.000	1.000
0064	517.4500.S	Negative Pressure Containment and Collection of Waste Materials (structure) 01. B-39-970	LS	1.000	1.000
0066	517.6001.S	Portable Decontamination Facility 01. B-39-970	EACH	1.000	1.000
0068	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	150.000	150.000
0070	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	792.000	792.000
0072	602.0405	Concrete Sidewalk 4-Inch	SF	4,843.000	4,843.000
0074	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	324.000	324.000
0076	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	44.000	44.000
0078	603.8000	Concrete Barrier Temporary Precast Delivered	LF	100.000	100.000
0080	603.8125	Concrete Barrier Temporary Precast Installed	LF	200.000	200.000

Estimate Of Quantities By Plan Sets

6054-05-75

Line	Item	Item Description	Unit	Total	Qty
0084	611.0420	Reconstructing Manholes	EACH	5.000	5.000
0086	611.0430	Reconstructing Inlets	EACH	19.000	19.000
0088	611.0530	Manhole Covers Type J	EACH	5.000	5.000
0090	611.0600	Inlet Covers Type A	EACH	18.000	18.000
0092	611.8110	Adjusting Manhole Covers	EACH	18.000	18.000
0094	611.8115	Adjusting Inlet Covers	EACH	4.000	4.000
0096	618.0100	Maintenance And Repair of Haul Roads (project) 01. 6054-05-75	EACH	1.000	1.000
0098	619.1000	Mobilization	EACH	0.500	0.500
0100	620.0100	Concrete Corrugated Median	SF	72.000	72.000
0102	620.0300	Concrete Median Sloped Nose	SF	6.000	6.000
0104	624.0100	Water	MGAL	1.000	1.000
0106	625.0100	Topsoil	SY	252.000	252.000
0108	628.1905	Mobilizations Erosion Control	EACH	1.000	1.000
0110	628.1910	Mobilizations Emergency Erosion Control	EACH	1.000	1.000
0112	628.2006	Erosion Mat Urban Class I Type A	SY	2,261.000	2,261.000
0114	628.7015	Inlet Protection Type C	EACH	75.000	75.000
0116	629.0210	Fertilizer Type B	CWT	0.160	0.160
0118	630.0130	Seeding Mixture No. 30	LB	5.000	5.000
0120	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	82.000	82.000
0122	637.2210	Signs Type II Reflective H	SF	573.000	573.000
0124	637.2230	Signs Type II Reflective F	SF	123.000	123.000
0126	638.2102	Moving Signs Type II	EACH	12.000	12.000
0128	638.2602	Removing Signs Type II	EACH	80.000	80.000
0130	638.3000	Removing Small Sign Supports	EACH	84.000	84.000
0132	642.5201	Field Office Type C	EACH	1.000	1.000
0134	643.0300	Traffic Control Drums	DAY	2,588.000	2,588.000
0138	643.0410	Traffic Control Barricades Type II	DAY	1,344.000	1,344.000
0140	643.0420	Traffic Control Barricades Type III	DAY	1,794.000	1,794.000
0142	643.0705	Traffic Control Warning Lights Type A	DAY	814.000	814.000
0144	643.0715	Traffic Control Warning Lights Type C	DAY	1,858.000	1,858.000
0146	643.0900	Traffic Control Signs	DAY	7,141.000	7,141.000
0148	643.0920	Traffic Control Covering Signs Type II	EACH	6.000	6.000
0150	643.5000	Traffic Control	EACH	0.500	0.500
0152	646.1020	Marking Line Epoxy 4-Inch	LF	4,032.000	4,032.000
0154	646.3020	Marking Line Epoxy 8-Inch	LF	345.000	345.000
0156	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	14,234.000	14,234.000
0158	646.5020	Marking Arrow Epoxy	EACH	3.000	3.000
0160	646.5120	Marking Word Epoxy	EACH	1.000	1.000
0162	646.6120	Marking Stop Line Epoxy 18-Inch	LF	296.000	296.000

Estimate Of Quantities By Plan Sets

6054-05-75

Line	Item	Item Description	Unit	Total	Qty
0164	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	1,631.000	1,631.000
0166	646.8020	Marking Corrugated Median Epoxy	SF	17.000	17.000
0168	646.8120	Marking Curb Epoxy	LF	44.000	44.000
0170	646.8220	Marking Island Nose Epoxy	EACH	3.000	3.000
0172	646.9000	Marking Removal Line 4-Inch	LF	500.000	500.000
0174	646.9300	Marking Removal Special Marking	EACH	2.000	2.000
0176	649.0105	Temporary Marking Line Paint 4-Inch	LF	14,234.000	14,234.000
0178	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	980.000	980.000
0180	649.0250	Temporary Marking Line Removable Tape 8-Inch	LF	290.000	290.000
0182	649.0550	Temporary Marking Arrow Removable Tape	EACH	4.000	4.000
0184	649.0650	Temporary Marking Word Removable Tape	EACH	2.000	2.000
0186	649.0850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	88.000	88.000
0188	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	942.000	942.000
0190	650.8000	Construction Staking Resurfacing Reference	LF	7,630.000	7,630.000
0192	650.9000	Construction Staking Curb Ramps	EACH	38.000	38.000
0194	650.9910	Construction Staking Supplemental Control (project) 01. 6054-05-75	LS	1.000	1.000
0196	653.0900	Adjusting Pull Boxes	EACH	1.000	1.000
0198	654.0101	Concrete Bases Type 1	EACH	1.000	1.000
0200	657.0100	Pedestal Bases	EACH	1.000	1.000
0202	657.0430	Traffic Signal Standards Aluminum 10-FT	EACH	1.000	1.000
0204	658.0416	Pedestrian Signal Face 16-Inch	EACH	1.000	1.000
0206	658.0500	Pedestrian Push Buttons	EACH	1.000	1.000
0208	690.0150	Sawing Asphalt	LF	862.000	862.000
0210	690.0250	Sawing Concrete	LF	2,863.000	2,863.000
0212	715.0415	Incentive Strength Concrete Pavement	DOL	500.000	500.000
0214	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0216	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000
0224	SPV.0060	Special 01. Adjusting Water Valve Boxes	EACH	14.000	14.000
0226	SPV.0060	Special 02. Remove and Salvage Pedestrian Signal Head	EACH	1.000	1.000
0228	SPV.0060	Special 03. Remove Pedestrian Push Button	EACH	1.000	1.000
0232	SPV.0105	Special 01. Temporary Traffic Signals for Intersection	LS	1.000	1.000
0234	SPV.0105	Special 02. Temporary Vehicle Detection STH 22/23 Intersection	LS	1.000	1.000
0236	SPV.0165	Special 01. Concrete Cure and Seal Treatment	SF	333.000	333.000
0238	SPV.0180	Special 01. Crushed Red Granite Mulch	SY	27.000	27.000

Estimate Of Quantities By Plan Sets

6054-05-75

REMOVING PAVEMENT

204.0100				
STA:	STA:	LOCATION	SY	REMARKS
678+64	- 10"L"+43	LT	6	CASS STREET CURB RAMP NW QUADRANT
10"L"+48	- 679+33	LT	4	CASS STREET CURB RAMP NE QUADRANT
678+64	- 678+78	RT	2	CASS STREET CURB RAMP SW QUADRANT
679+22	- 679+35	RT	1	CASS STREET CURB RAMP SE QUADRANT
686+80	- 10"M"+36	LT	3	EAST STREET CURB RAMP NW QUADRANT
10"M"+36	- 687+50	LT	4	EAST STREET CURB RAMP NE QUADRANT
687+34	- 687+48	RT	2	EAST STREET CURB RAMP SE QUADRANT
690+12	- 690+69	LT	2	CHURCH STREET CURB RAMP NW QUADRANT
690+70	- 10"N"+55	LT	4	CHURCH STREET CURB RAMP NE QUADRANT
10"N"+39	- 10"N"+55	RT	2	CHURCH STREET CURB RAMP SE QUADRANT
666+94	- 668+78	LT & RT	563	STH 22/23 - STAGE ONE
9"K"+51	- 9"K"+58	LT & RT	5	STH 22/23 - STAGE ONE
669+30	- 669+46	LT & RT	12	STH 22/23 - STAGE ONE
667+16	- 669+94	LT & RT	334	STH 22/23 - STAGE TWO
670+07	- 670+34	LT & RT	34	STH 22/23 - STAGE TWO
9"K"+52	- 9"K"+58	LT	10	STH 22/23 - STAGE THREE
668+79	- 669+13	LT	57	STH 22/23 - STAGE THREE
669+33	- 669+74	LT	68	STH 22/23 - STAGE THREE
669+87	- 669+93	LT	10	STH 22/23 - STAGE THREE
670+07	- 670+34	LT	45	STH 22/23 - STAGE THREE
666+25	- 666+62	LT	58	STH 22 - STAGE FOUR
665+79	- 666+13	LT	53	STH 22 - STAGE FOUR
664+40	- 664+55	LT	23	STH 22 - STAGE FOUR
662+75	- 662+81	RT	9	STH 22 - STAGE FIVE
672+00	- 672+28	RT	3	STH 22/23 CURB & GUTTER REPLACEMENT
674+10	- 674+65	RT	6	STH 22/23 CURB & GUTTER REPLACEMENT
679+50	- 679+78	RT	3	STH 22/23 CURB & GUTTER REPLACEMENT
680+05	- 680+25	RT	2	STH 22/23 CURB & GUTTER REPLACEMENT
687+80	- 688+00	LT	2	STH 22/23 CURB & GUTTER REPLACEMENT

PROJECT TOTAL: 1328

REMOVING CONCRETE SURFACE
PARITAL DEPTH

204.0109.S				
STATION -	STATION	LOCATION	SF	REMARKS
670+97	- 690+32	RT & LT	2444	CONCRETE DRIVEWAYS ON MONTELLO STREET
10"N"+00	- 16"N"+57	RT & LT	835	CONCRETE DRIVEWAYS ON STH 23

PROJECT TOTAL: 3278

REMOVING ASPHALTIC SURFACE

204.0120
204.0110 MILLING

CATEGORY	STATION -	STATION	LOCATION	SY	SY	REMARKS
0010	614+50	- 662+05	RT & LT	---	19871	MAINLINE
	670+97	- 690+81	RT & LT	---	6880	MAINLINE
	10"A"+24	- 11"A"+50	RT & LT	---	1059	5TH STREET
	10"A"+30	- 10"A"+47	RT	3	---	5TH STREET CURB RAMP
	7"B"+70	- 9"B"+76	RT & LT	---	881	COUNTY F
	9"B"+64	- 9"B"+82	LT	2	---	COUNTY F CURB RAMP
	625+15	- 625+26	LT	1	---	MID-BLOCK WEST CURB RAMP
	625+14	- 625+26	RT	1	---	MID-BLOCK EAST CURB RAMP
	625+09	- 625+18	40' RT	1	---	MID-BLOCK EAST CURB RAMP
	8"C"+00	- 9"C"+76	RT & LT	---	667	4TH STREET
	9"C"+56	- 9"C"+71	RT	2	---	4TH STREET CURB RAMP SW QUAD
	9"C"+56	- 9"C"+71	LT	2	---	4TH STREET CURB RAMP NW QUAD
	9"D"+20	- 9"D"+76	RT & LT	---	280	3RD STREET
	9"D"+63	- 9"D"+77	RT	2	---	3RD STREET CURB RAMP SW QUAD
	9"D"+63	- 9"D"+77	LT	2	---	3RD STREET CURB RAMP NW QUAD
	9"E"+00	- 9"E"+76	RT & LT	---	306	2ND STREET (WEST)
	9"E"+63	- 9"E"+77	RT	2	---	2ND STREET CURB RAMP SW QUAD
	9"E"+64	- 9"E"+83	LT	3	---	2ND STREET CURB RAMP NW QUAD
	10"E"+18	- 10"E"+55	RT & LT	---	171	2ND STREET (EAST)
	10"E"+25	- 10"E"+39	RT	2	---	2ND STREET CURB RAMP SE QUAD
	10"E"+18	- 10"E"+39	LT	3	---	2ND STREET CURB RAMP NE QUAD
	636+77	- 637+04	36' RT	6	---	2ND STREET CURB RAMP NE QUAD
	9"F"+35	- 9"F"+76	RT & LT	---	220	WATER STREET (WEST)
	9"F"+64	- 9"F"+78	RT	2	---	WATER STREET CURB RAMP SW QUAD
	9"F"+61	- 9"F"+74	LT	2	---	WATER STREET CURB RAMP NW QUAD
	10"F"+18	- 10"F"+50	RT & LT	---	125	WATER STREET (EAST)
	10"F"+27	- 10"F"+41	RT	2	---	WATER STREET CURB RAMP SE QUAD
	10"F"+23	- 10"F"+38	LT	2	---	WATER STREET CURB RAMP NE QUAD
	10"G"+18	- 10"G"+55	RT & LT	---	141	ISLAND DRIVE
	10"G"+25	- 10"G"+38	RT	2	---	ISLAND DRIVE CURB RAMP SE QUAD
	10"G"+23	- 10"G"+37	LT	2	---	ISLAND DRIVE CURB RAMP NE QUAD
	9"H"+55	- 9"H"+76	RT & LT	---	85	SUNSET DRIVE
	9"H"+61	- 9"H"+71	RT	1	---	SUNSET DRIVE CURB RAMP SW QUAD
	9"H"+69	- 9"H"+81	LT	1	---	SUNSET DRIVE CURB RAMP NW QUAD
	10"L"+22	- 10"L"+35	RT & LT	---	73	CASS STREET
	10"M"+22	- 10"M"+35	RT & LT	---	85	EAST STREET
	10"N"+00	- 16"N"+57	RT & LT	---	2336	STH 23 ("N" LINE)

CATEGORY 0010 TOTAL: 48 33160

204.0120
204.0110 MILLING

CATEGORY	STATION -	STATION	LOCATION	SY	SY	REMARKS
0030	670+97	- 690+32	RT & LT	---	2580	MAINLINE PARKING LANE
	10"N"+00	- 16"N"+57	RT & LT	---	876	MAINLINE PARKING LANE

CATEGORY 0030 TOTAL: 0 3456

PROJECT TOTAL: 48 36616

REMOVING CURB AND GUTTER

204.0150

STATION - STATION	LOCATION	LF	REMARKS
10"A"+30 - 10"A"+47	LT	27	5TH STREET CURB RAMP
9"B"+64 - 9"B"+82	LT	22	COUNTY F CURB RAMP
625+15 - 625+26	LT	11	STH 22 MID-BLOCK CURB RAMP
625+14 - 625+26	RT	12	STH 22 MID-BLOCK CURB RAMP
9"C"+56 - 9"C"+71	RT	18	4TH STREET SW CURB RAMP
9"C"+56 - 9"C"+71	LT	18	4TH STREET NW CURB RAMP
9"D"+63 - 9"D"+77	RT	18	3RD STREET SW CURB RAMP
9"D"+63 - 9"D"+77	LT	18	3RD STREET NW CURB RAMP
9"E"+63 - 9"E"+77	RT	18	2ND STREET SW CURB RAMP
9"E"+64 - 9"E"+83	LT	29	2ND STREET NW CURB RAMP
10"E"+18 - 10"E"+39	LT	30	2ND STREET NE CURB RAMP
10"E"+25 - 10"E"+39	RT	18	2ND STREET SE CURB RAMP
9"F"+64 - 9"F"+74	RT	17	WATER STREET SW CURB RAMP
9"F"+61 - 9"F"+74	LT	18	WATER STREET NW CURB RAMP
10"F"+27 - 10"F"+41	RT	18	WATER STREET SE CURB RAMP
10"F"+23 - 10"F"+38	LT	18	WATER STREET NE CURB RAMP
10"G"+25 - 10"G"+38	RT	20	ISLAND DRIVE SE CURB RAMP
10"G"+23 - 10"G"+37	LT	18	ISLAND DRIVE NE CURB RAMP
9"H"+61 - 9"H"+71	RT	13	SUNSET DRIVE SW CURB RAMP
9"H"+69 - 9"H"+81	LT	12	SUNSET DRIVE SW CURB RAMP
668+37 - 668+56	LT	19	STH 22/23 SW CURB RAMP
9"K"+42 - 9"K"+62	RT	20	STH 22/23 SW CURB RAMP
9"K"+46 - 9"K"+60	LT	14	STH 22/23 NW CURB RAMP
668+37 - 668+48	RT	11	STH 22/23 ISLAND MEDIAN
669+37 - 669+50	RT	13	STH 22/23 ISLAND MEDIAN
669+43 - 669+59	RT	16	STH 22/23 ISLAND MEDIAN
669+58 - 669+85	RT	33	STH 22/23 SE CURB RAMP
669+32 - 669+56	LT	24	STH 22/23 NE CURB RAMP
678+64 - 10"L"+43	LT	55	CASS STREET NW CURB RAMP
10"L"+48 - 679+33	LT	40	CASS STREET NE CURB RAMP
678+64 - 678+78	RT	14	CASS STREET SW CURB RAMP
679+21 - 679+35	RT	14	CASS STREET SE CURB RAMP
686+80 - 10"M"+36	LT	23	EAST STREET NW CURB RAMP
10"M"+36 - 687+50	LT	33	EAST STREET NE CURB RAMP
687+34 - 687+48	RT	14	EAST STREET SE CURB RAMP
690+11 - 690+69	LT	20	CHURCH STREET NW CURB RAMP
10"N"+28 - 10"N"+55	LT	35	CHURCH STREET NE CURB RAMP
10"N"+35 - 10"N"+55	RT	20	CHURCH STREET SE CURB RAMP
672+00 - 672+28	RT	28	MAINLINE
674+10 - 674+65	RT	55	MAINLINE
679+50 - 679+78	RT	28	MAINLINE
680+05 - 680+25	RT	20	MAINLINE
687+80 - 688+00	LT	20	MAINLINE

PROJECT TOTAL: 942

REMOVING CONCRETE SIDEWALK

204.0155

STATION - STATION	LOCATION	SY	REMARKS
621+65 - 621+93	RT	13	5TH STREET NE CURB RAMP
624+57 - 624+75	LT	12	COUNTY F CURB RAMP
625+15 - 625+27	LT	15	STH 22 MID-BLOCK CURB RAMP
625+10 - 625+26	RT	15	STH 22 MID-BLOCK CURB RAMP
627+80 - 628+04	LT	13	4TH STREET SW CURB RAMP
628+47 - 628+71	LT	13	4TH STREET NW CURB RAMP
631+96 - 632+20	LT	13	3RD STREET SW CURB RAMP
632+63 - 632+83	LT	12	3RD STREET NW CURB RAMP
636+12 - 636+37	LT	13	2ND STREET SW CURB RAMP
636+03 - 636+36	RT	19	2ND STREET SE CURB RAMP
636+80 - 637+04	LT	16	2ND STREET NW CURB RAMP
636+79 - 637+03	RT	17	2ND STREET NE CURB RAMP
640+43 - 640+72	LT	16	WATER STREET SW CURB RAMP
640+40 - 640+70	RT	17	WATER STREET SE CURB RAMP
641+18 - 641+45	LT	15	WATER STREET NW CURB RAMP
641+06 - 641+28	RT	12	WATER STREET NE CURB RAMP
644+83 - 645+11	RT	15	ISLAND DRIVE SE CURB RAMP
645+48 - 645+73	RT	13	ISLAND DRIVE NE CURB RAMP
647+24 - 647+49	LT	20	SUNSET DRIVE SW CURB RAMP
647+80 - 647+92	LT	7	SUNSET DRIVE NW CURB RAMP
668+37 - 668+54	LT	14	STH 22/23 SW CURB RAMP
9"K"+42 - 9"K"+62	RT	14	STH 22/23 SW CURB RAMP
9"K"+46 - 9"K"+60	LT	10	STH 22/23 NW CURB RAMP
668+40 - 669+49	RT	29	STH 22/23 ISLAND MEDIAN CURB RAMPS
669+63 - 669+92	RT	24	STH 22/23 SE CURB RAMP
669+13 - 669+57	LT	15	STH 22/23 NE CURB RAMP
678+64 - 678+79	LT	8	CASS STREET NW CURB RAMP
678+88 - 10"L"+43	LT	9	CASS STREET NW CURB RAMP
10"L"+49 - 679+33	LT	13	CASS STREET NE CURB RAMP
678+64 - 678+80	RT	11	CASS STREET SW CURB RAMP
679+18 - 679+33	RT	11	CASS STREET SE CURB RAMP
686+63 - 686+91	LT	18	EAST STREET NW CURB RAMP
687+31 - 687+51	LT	18	EAST STREET NE CURB RAMP
687+37 - 687+48	RT	10	EAST STREET SE CURB RAMP
690+02 - 690+69	LT	14	CHURCH STREET NW CURB RAMP
10"N"+32 - 10"N"+54	LT	20	CHURCH STREET NE CURB RAMP
10"N"+43 - 10"N"+54	RT	8	CHURCH STREET SE CURB RAMP

PROJECT TOTAL: 532

FINISHING ROADWAY (PROJECT)

STATION - STATION	LOCATION	213.0100 01. (6054-05-75) EACH
614+50 - 690+81	LT & RT	1
PROJECT TOTAL:		1

BASE AGGREGATE DENSE

STATION - STATION	LOCATION	305.0115 3/4-INCH	305.0125 1 1/4-INCH	REMARKS
614+50 - 620+39	RT	10	---	MAINLINE
614+50 - 623+63	LT	15	---	MAINLINE
662+75 - 670+34	RT & LT	---	12	UNDISTRIBUTED
PROJECT TOTALS:		25	12	

SHAPING SHOULDERS

STATION - STATION	LOCATION	305.0500 STA	REMARKS
614+50 - 620+50	RT	6	MAINLINE
614+50 - 624+50	LT	10	MAINLINE
PROJECT TOTAL:		16	

BASE PATCHING

STATION - STATION	LOCATION	390.0303 CONCRETE SY	REMARKS
671+00 - 690+81	LT & RT	500	MAINLINE
PROJECT TOTAL:		500	

3

3

CONCRETE PAVEMENT

416.0610 416.0620
 DRILLED DRILLED
 415.0080 TIE DOWEL
 8-INCH BARS BARS

STA:	STA:	LOCATION	SY	EACH	EACH	REMARKS
666+94	- 668+78	LT & RT	552	58.1	204	STH 22/23 - STAGE ONE
9"K"+51	- 9"K"+58	LT & RT	5	3.3	0	STH 22/23 - STAGE ONE
669+30	- 669+46	LT & RT	10	11.4	52	STH 22/23 - STAGE ONE
667+16	- 669+94	LT & RT	386	47.1	53	STH 22/23 - STAGE TWO
670+07	- 670+34	LT & RT	48	8.3	0	STH 22/23 - STAGE TWO
9"K"+52	- 9"K"+58	LT	10	---	31	STH 22/23 - STAGE THREE
668+79	- 669+13	LT	56	---	12	STH 22/23 - STAGE THREE
669+33	- 669+50	LT	66	9.3	113	STH 22/23 - STAGE THREE
669+87	- 669+93	LT	9	---	17	STH 22/23 - STAGE THREE
670+07	- 670+34	LT	41	---	0	STH 22/23 - STAGE THREE
666+25	- 666+62	LT	57	7	22	STH 22 - STAGE FOUR
665+79	- 666+13	LT	51	7	22	STH 22 - STAGE FOUR
664+40	- 664+55	LT	23	7	22	STH 22 - STAGE FOUR
662+75	- 662+81	RT	8	7	22	STH 22 - STAGE FIVE
PROJECT TOTALS:			1322	166	570	

**REHEATING HMA PAVEMENT
 LONGITUDINAL JOINTS**

460.4110.S
 LF

STA:	STA:	CL	LF
614+50	- 643+12	CL	2862
644+22	- 646+70	CL	248
647+30	- 650+57	CL	327
651+33	- 662+05	CL	1072
670+97	- 690+32	CL	1935
10"N"+00	- 16"N"+57	CL	57
PROJECT TOTAL:			6501

ASPHALTIC SURFACE

465.0105

STATION - STATION	LOCATION	TON	REMARKS
625+09 - 625+18	40' RT	0.10	MID-CLOCK EAST CURB RAMP
636+77 - 637+04	36' RT	0.60	2ND STREET CURB RAMP NE QUADRANT
		0.30	UNDISTRIBUTED
PROJECT TOTAL:			1

TACK COAT

455.0605

STATION - STATION	LOCATION	GAL	REMARKS
CATEGORY 0010			
614+50 - 643+12	RT & LT	927	MAINLINE & DRIVEWAYS
644+22 - 646+70	RT & LT	69	MAINLINE
647+30 - 650+74	RT & LT	92	MAINLINE
651+33 - 662+05	RT & LT	303	MAINLINE
670+97 - 690+81	RT & LT	516	MAINLINE & DRIVEWAYS
10"A"+24 - 11"A"+50	RT & LT	74	5TH STREET
7"B"+70 - 9"B"+76	RT & LT	62	COUNTY F
8"C"+00 - 9"C"+76	RT & LT	5	4TH STREET
9"D"+20 - 9"D"+76	RT & LT	18	3RD STREET
9"E"+00 - 9"E"+76	RT & LT	21	2ND STREET (WEST)
10"E"+18 - 10"E"+55	RT & LT	12	2ND STREET (EAST)
9"F"+35 - 9"F"+76	RT & LT	15	WATER STREET (WEST)
10"F"+18 - 10"F"+50	RT & LT	9	WATER STREET (EAST)
10"G"+18 - 10"G"+55	RT & LT	10	ISLAND DRIVE
9"H"+55 - 9"H"+76	RT & LT	6	SUNSET DRIVE
10"L"+22 - 10"L"+35	RT & LT	5	CASS STREET
10"M"+22 - 10"M"+35	RT & LT	6	EAST STREET
10"N"+00 - 16"N"+57	RT & LT	164	STH 23 ("N" LINE)
CATEGORY 0010 TOTAL:			2314
CATEGORY 0030			
670+97 - 690+32	RT & LT	181	MAINLINE & DRIVEWAYS
10"N"+00 - 16"N"+57	RT & LT	61	
CATEGORY 0030 TOTAL:			242
PROJECT TOTAL:			2556

HMA PAVEMENT

460.5224
 4 LT
 58-28 S

STATION - STATION	LOCATION	TON	REMARKS
CATEGORY 0010			
614+50 - 690+81	RT & LT	2672	MAINLINE
614+50 - 662+05	LT	17	MAIN STREET PRIVATE ENTRANCES
670+97 - 690+81	LT & RT	36	MONTELLO STREET PRIVATE ENTRANCES
10"A"+24 - 11"A"+50	RT & LT	54	5TH STREET
7"B"+70 - 9"B"+76	RT & LT	90	COUNTY F
8"C"+00 - 9"C"+76	RT & LT	67	4TH STREET
9"D"+20 - 9"D"+76	RT & LT	26	3RD STREET
9"E"+00 - 9"E"+76	RT & LT	30	2ND STREET (WEST)
10"E"+18 - 10"E"+55	RT & LT	18	2ND STREET (EAST)
9"F"+35 - 9"F"+76	RT & LT	23	WATER STREET (WEST)
10"F"+18 - 10"F"+50	RT & LT	13	WATER STREET (EAST)
10"G"+18 - 10"G"+55	RT & LT	15	ISLAND DRIVE
9"H"+55 - 9"H"+76	RT & LT	9	SUNSET DRIVE
10"L"+22 - 10"L"+35	RT & LT	8	CASS STREET
10"M"+22 - 10"M"+35	RT & LT	9	EAST STREET
10"N"+00 - 16"N"+57	RT	160	STH 23 ("N" LINE)
10"N"+00 - 16"N"+57	LT	160	
CATEGORY 0010 TOTAL:			3407
CATEGORY 0030			
670+97 - 690+32	RT & LT	258	PARKING LANE
CATEGORY 0030 TOTAL:			258
PROJECT TOTAL:			3665

CONCRETE CURB AND GUTTER

601.0409 601.0411
30-INCH 30-INCH
TYPE A TYPE D

STATION - STATION	LOCATION	LF	LF	REMARKS
10"A"+30 - 10"A"+47	LT	---	27	5TH STREET CURB RAMP
9"B"+64 - 9"B"+82	LT	---	22	COUNTY F CURB RAMP
625+15 - 625+26	LT	---	11	STH 22 MID-BLOCK CURB RAMP
625+14 - 625+26	RT	---	12	STH 22 MID-BLOCK CURB RAMP
9"C"+56 - 9"C"+71	RT	---	18	4TH STREET SW CURB RAMP
9"C"+56 - 9"C"+71	LT	---	18	4TH STREET NW CURB RAMP
9"D"+63 - 9"D"+77	RT	---	18	3RD STREET SW CURB RAMP
9"D"+63 - 9"D"+77	LT	---	18	3RD STREET NW CURB RAMP
9"E"+63 - 9"E"+77	RT	---	18	2ND STREET SW CURB RAMP
9"E"+64 - 9"E"+83	LT	---	29	2ND STREET NW CURB RAMP
10"E"+18 - 10"E"+39	LT	---	30	2ND STREET NE CURB RAMP
10"E"+25 - 10"E"+39	RT	---	18	2ND STREET SE CURB RAMP
9"F"+64 - 9"F"+74	RT	---	17	WATER STREET SW CURB RAMP
9"F"+61 - 9"F"+74	LT	---	18	WATER STREET NW CURB RAMP
10"F"+27 - 10"F"+41	RT	---	18	WATER STREET SE CURB RAMP
10"F"+23 - 10"F"+38	LT	---	18	WATER STREET NE CURB RAMP
10"G"+25 - 10"G"+38	RT	---	20	ISLAND DRIVE SE CURB RAMP
10"G"+23 - 10"G"+37	LT	---	18	ISLAND DRIVE NE CURB RAMP
9"H"+61 - 9"H"+71	RT	---	13	SUNSET DRIVE SW CURB RAMP
9"H"+69 - 9"H"+81	LT	---	12	SUNSET DRIVE SW CURB RAMP
668+37 - 668+56	LT	19	---	STH 22/23 SW CURB RAMP
9"K"+42 - 9"K"+62	RT	20	---	STH 22/23 SW CURB RAMP
9"K"+46 - 9"K"+60	LT	14	---	STH 22/23 NW CURB RAMP
668+37 - 668+48	RT	11	---	STH 22/23 ISLAND MEDIAN
669+37 - 669+50	RT	13	---	STH 22/23 ISLAND MEDIAN
669+43 - 669+59	RT	16	---	STH 22/23 ISLAND MEDIAN
669+58 - 669+85	RT	33	---	STH 22/23 SE CURB RAMP
669+32 - 669+56	LT	24	---	STH 22/23 NE CURB RAMP
678+64 - 10"L"+43	LT	---	55	CASS STREET NW CURB RAMP
10"L"+48 - 679+33	LT	---	40	CASS STREET NE CURB RAMP
678+64 - 678+78	RT	---	14	CASS STREET SW CURB RAMP
679+21 - 679+35	RT	---	14	CASS STREET SE CURB RAMP
686+80 - 10"M"+36	LT	---	23	EAST STREET NW CURB RAMP
10"M"+36 - 687+50	LT	---	33	EAST STREET NE CURB RAMP
687+34 - 687+48	RT	---	14	EAST STREET SE CURB RAMP
690+11 - 690+69	LT	---	20	CHURCH STREET NW CURB RAMP
10"N"+28 - 10"N"+55	LT	---	35	CHURCH STREET NE CURB RAMP
10"N"+35 - 10"N"+55	RT	---	20	CHURCH STREET SE CURB RAMP
672+00 - 672+28	RT	---	28	MAINLINE
674+10 - 674+65	RT	---	55	MAINLINE
679+50 - 679+78	RT	---	28	MAINLINE
680+05 - 680+25	RT	---	20	MAINLINE
687+80 - 688+00	LT	---	20	MAINLINE

PROJECT TOTALS: 150 792

CONCRETE SIDEWALK

602.0605
602.0505 CURB RAMP SPV.0165.01
405.1000 STAMPING
COLORED 602.0405 CURB RAMP DETECTABLE CONCRETE
CONCRETE 4-INCH WARNING FIELD RADIAL CURE AND
YELLOW YELLOW SEAL
TREATMENT

STATION - STATION	LOCATION	CY	SF	SF	SF	SF	REMARKS
621+65 - 621+93	RT	---	121	---	20	---	5TH STREET NE CURB RAMP
624+57 - 624+75	LT	---	108	---	24	---	COUNTY F CURB RAMP
625+15 - 625+27	LT	---	138	8	---	---	STH 22 MID-BLOCK CURB RAMP
625+10 - 625+26	RT	---	141	8	---	---	STH 22 MID-BLOCK CURB RAMP
627+80 - 628+04	LT	---	118	8	---	---	4TH STREET SW CURB RAMP
628+47 - 628+71	LT	---	117	8	---	---	4TH STREET NW CURB RAMP
631+96 - 632+20	LT	---	121	8	---	---	3RD STREET SW CURB RAMP
632+63 - 632+83	LT	---	107	8	---	---	3RD STREET NW CURB RAMP
636+12 - 636+37	LT	---	121	8	---	---	2ND STREET SW CURB RAMP
636+03 - 636+36	RT	---	169	8	---	---	2ND STREET SE CURB RAMP
636+80 - 637+04	LT	---	146	16	---	---	2ND STREET NW CURB RAMP
636+79 - 637+03	RT	---	157	16	---	---	2ND STREET NE CURB RAMP
640+43 - 640+72	LT	---	147	8	---	---	WATER STREET SW CURB RAMP
640+40 - 640+70	RT	---	151	8	---	---	WATER STREET SE CURB RAMP
641+18 - 641+45	LT	---	134	8	---	---	WATER STREET NW CURB RAMP
641+06 - 641+28	RT	---	110	8	---	---	WATER STREET NE CURB RAMP
644+83 - 645+11	RT	---	139	8	---	---	ISLAND DRIVE SE CURB RAMP
645+48 - 645+73	RT	---	122	8	---	---	ISLAND DRIVE NE CURB RAMP
647+24 - 647+49	LT	---	178	8	---	---	SUNSET DRIVE SW CURB RAMP
647+80 - 647+92	LT	---	66	8	---	---	SUNSET DRIVE NW CURB RAMP
668+37 - 668+54	LT	0.35	132	8	---	24	STH 22/23 SW CURB RAMP
9"K"+42 - 9"K"+62	RT	0.29	127	8	---	28	STH 22/23 SW CURB RAMP
9"K"+46 - 9"K"+60	LT	0.25	92	8	---	21	STH 22/23 NW CURB RAMP
668+40 - 669+49	RT	2.02	258	24	---	165	STH 22/23 ISLAND MEDIAN CURB RAMPS
669+63 - 669+92	RT	0.64	233	8	---	53	STH 22/23 SE CURB RAMP
669+13 - 669+57	LT	0.51	135	8	---	42	STH 22/23 NE CURB RAMP
678+64 - 678+79	LT	---	71	8	---	---	CASS STREET NW CURB RAMP
678+88 - 10"L"+43	LT	---	81	8	---	---	CASS STREET NW CURB RAMP
10"L"+49 - 679+33	LT	---	123	8	---	---	CASS STREET NE CURB RAMP
678+64 - 678+80	RT	---	95	8	---	---	CASS STREET SW CURB RAMP
679+18 - 679+33	RT	---	100	8	---	---	CASS STREET SE CURB RAMP
686+63 - 686+91	LT	---	162	8	---	---	EAST STREET NW CURB RAMP
687+31 - 687+51	LT	---	159	16	---	---	EAST STREET NE CURB RAMP
687+37 - 687+48	RT	---	77	8	---	---	EAST STREET SE CURB RAMP
690+02 - 690+69	LT	---	144	6	---	---	CHURCH STREET NW CURB RAMP
10"N"+32 - 10"N"+54	LT	---	177	14	---	---	CHURCH STREET NE CURB RAMP
10"N"+43 - 10"N"+54	RT	---	68	8	---	---	CHURCH STREET SE CURB RAMP

PROJECT TOTALS: 4 4843 324 44 333

INLETS, MANHOLES, WATER VALVE AND PULL BOXES

LABEL NO.	STATION	LOCATION	611.0420	611.0430	611.0530	611.0600	611.8110	611.8115	653.0900	SPV.0060.01	REMARKS
			RECONSTRUCTING MANHOLES CATEGORY 0010 EACH	RECONSTRUCTING INLETS CATEGORY 0010 EACH	MANHOLE COVERS TYPE J CATEGORY 0010 EACH	INLET COVERS TYPE A CATEGORY 0010 EACH	ADJUSTING MANHOLE COVERS CATEGORY 0010 EACH	ADJUSTING INLET COVERS CATEGORY 0030 EACH	ADJUSTING PULL BOXES CATEGORY 0010 EACH	ADJUSTING WATER VALVE BOXES CATEGORY 0030 EACH	
1-01	621+59	RT	---	---	---	---	---	1	---	---	EXISTING COVER
3-01	632+59	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
3-02	634+45	RT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
3-03	636+76	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
4-01	640+80	RT	---	---	---	---	---	---	---	1	EXISTING COVER
4-02	640+83	LT	---	---	---	---	---	---	---	1	EXISTING COVER
4-03	641+12	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
5-01	652+20	LT	---	---	---	---	---	---	---	1	EXISTING COVER
6-01	655+56	LT	---	---	---	---	---	---	---	1	EXISTING COVER
6-02	658+20	LT	---	---	---	---	---	---	---	1	EXISTING COVER
6-03	658+28	LT	---	---	---	---	---	---	---	1	EXISTING COVER
7-01	666+36	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
7-02	668+30	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
7-03	668+46	LT	---	---	---	---	---	---	---	1	EXISTING COVER
7-04	669+51	RT	---	---	---	---	---	---	1	---	EXISTING COVER
7-05	670+15	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
7-06	670+20	LT	---	1	---	---	---	---	---	---	INSIDE REPAIR ONLY; DO NOT RESET INLET COVER
8-01	674+75	RT	---	1	---	1	---	---	---	---	
8-02	675+00	LT	1	---	1	---	---	---	---	---	
8-03	675+00	LT	---	1	---	1	---	---	---	---	
8-04	676+60	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
8-05	676+80	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
8-06	676+80	LT	---	1	---	1	---	---	---	---	
8-07	676+85	RT	---	1	---	1	---	---	---	---	
9-01	678+90	RT	---	1	---	1	---	---	---	---	
9-02	678+90	LT	---	1	---	1	---	---	---	---	
9-03	679+03	LT	1	---	1	---	---	---	---	---	
9-04	10"L"+32	LT	---	---	---	---	---	1	---	---	EXISTING COVER
9-05	10"L"+32	RT	---	---	---	---	---	1	---	---	EXISTING COVER
9-06	679+25	LT	---	---	---	---	---	1	---	---	EXISTING COVER

continued...

INLETS, MANHOLES, WATER VALVE AND PULL BOXES

LABEL NO.	STATION	LOCATION	611.0420	611.0430	611.0530	611.0600	611.8110	611.8115	653.0900	SPV.0060.01	REMARKS
			RECONSTRUCTING MANHOLES CATEGORY 0010 EACH	RECONSTRUCTING INLETS CATEGORY 0010 EACH	MANHOLE COVERS TYPE J CATEGORY 0010 EACH	INLET COVERS TYPE A CATEGORY 0010 EACH	ADJUSTING MANHOLE COVERS CATEGORY 0010 EACH	ADJUSTING INLET COVERS CATEGORY 0030 EACH	ADJUSTING PULL BOXES CATEGORY 0010 EACH	ADJUSTING WATER VALVE BOXES CATEGORY 0030 EACH	
9-07	679+10	LT	---	---	---	---	---	1	---	---	EXISTING MANHOLE COVER
9-08	679+25	RT	---	---	---	---	---	---	---	1	EXISTING COVER
9-09	682+40	LT	---	---	---	---	---	1	---	---	EXISTING MANHOLE COVER
9-10	682+75	LT	---	1	---	1	---	---	---	---	
9-11	682+75	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
9-12	682+75	RT	---	1	---	1	---	---	---	---	
9-13	685+40	LT	---	---	---	---	---	1	---	---	EXISTING MANHOLE COVER
9-14	685+40	RT	---	---	---	---	---	1	---	---	EXISTING MANHOLE COVER
10-01	686+90	RT	---	1	---	1	---	---	---	---	
10-02	686+90	LT	1	---	1	---	---	---	---	---	
10-03	686+81	LT	---	1	---	1	---	---	---	---	
10-04	687+30	RT	---	---	---	---	---	1	---	---	EXISTING MANHOLE COVER
10-05	687+38	LT	---	1	---	1	---	---	---	---	
10-06	690+10	RT	---	---	---	---	---	---	---	1	EXISTING COVER
10-07	690+60	LT	---	1	---	1	---	---	---	---	
10-08	690+47	LT	1	---	1	---	---	---	---	---	
10-09	10"N"+22	RT	---	---	---	---	---	1	---	---	EXISTING MANHOLE COVER
10-10	10"N"+37	LT	---	1	---	1	---	---	---	---	
10-11	10"N"+44	RT	---	1	---	1	---	---	---	---	
10-12	11"N"+44	RT	---	---	---	---	---	---	---	1	EXISTING COVER
10-13	12"N"+69	RT	---	1	---	1	---	---	---	---	
10-14	12"N"+70	LT	---	1	---	1	---	---	---	---	
10-15	12"N"+70	LT	1	---	1	---	---	---	---	---	
11-01	13"N"+50	RT	---	---	---	---	---	1	---	---	EXISTING MANHOLE COVER
11-02	14"N"+21	RT	---	---	---	---	---	---	---	1	EXISTING COVER
11-03	14"N"+32	RT	---	---	---	---	---	---	---	1	EXISTING COVER
11-04	15"N"+13	RT	---	1	---	1	---	---	---	---	
11-05	15"N"+14	LT	---	---	---	---	1	---	---	---	EXISTING MANHOLE COVER
11-06	15"N"+14	LT	---	1	---	1	---	---	---	---	
11-07	15"N"+54	RT	---	---	---	---	---	---	---	1	EXISTING COVER
11-08	16"N"+20	RT	---	---	---	---	---	---	---	1	EXISTING COVER
CATEGORY 0010 SUBTOTAL:			5	19	5	18	8	10	4	1	
CATEGORY 0030 SUBTOTAL:											14
PROJECT TOTALS:			5	19	5	18	18	10	4	1	14

MAINTENANCE AND REPAIR
OF HAUL ROADS

618.0100 01. (6054-05-75)	
PROJECT	EACH
6054-05-75	1
PROJECT TOTAL:	1

MOBILIZATION

619.1000 EACH	
PROJECT	EACH
6054-05-75	0.5
PROJECT TOTAL:	0.5

CONCRETE MEDIAN

STATION - STATION	LOCATION	620.0100	620.0300	REMARKS
		CORRUGATED SF	SLOPED NOSE SF	
668+38 - 668+40	MEDIAN	---	6	TYPE 2
667+38 - 667+55	MEDIAN	72	---	
PROJECT TOTALS:		72	6	

WATER

STATION - STATION	LOCATION	624.0100 MGAL	REMARKS
614+50 - 690-81	LT & RT	1	UNDISTRIBUTED
PROJECT TOTAL:		1	

MOBILIZATIONS EROSION CONTROL

STATION - STATION	628.1905	628.1910
	MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
614+50 - 690+81	1	1
PROJECT TOTAL:	1	1

TOPSOIL, EROSION MAT, FERTILIZER, AND SEEDING

STATION - STATION	LOCATION	628.2006		630.0130		REMARKS
		625.0100	629.0210	629.0210	630.0130	
		EROSION MAT	FERTILIZER	SEEDING	MIXTURE	
		CLASS I	TYPE B	TYPE C	NO. 30	
		TYPE A	CWT	LB		
STATION - STATION	LOCATION	SY	SY	CWT	LB	REMARKS
621+65 - 621+93	RT	12	112	0.0078	0.22	5TH STREET NE CURB RAMP
624+57 624+75	LT	10	92	0.0065	0.18	COUNTY F CURB RAMP
625+15 625+27	LT	13	117	0.0082	0.23	STH 22 MID-BLOCK CURB RAMP
625+10 625+26	RT	11	95	0.0066	0.19	STH 22 MID-BLOCK CURB RAMP
627+80 628+04	LT	11	98	0.0068	0.20	4TH STREET SW CURB RAMP
628+47 628+71	LT	10	88	0.0062	0.18	4TH STREET NW CURB RAMP
631+96 632+20	LT	11	95	0.0067	0.19	3RD STREET SW CURB RAMP
632+63 632+83	LT	9	78	0.0054	0.16	3RD STREET NW CURB RAMP
636+12 636+37	LT	4	38	0.0026	0.08	2ND STREET SW CURB RAMP
636+03 636+36	RT	16	144	0.0101	0.29	2ND STREET SE CURB RAMP
636+80 637+04	LT	5	47	0.0033	0.09	2ND STREET NW CURB RAMP
636+79 637+03	RT	5	46	0.0032	0.09	2ND STREET NE CURB RAMP
640+43 640+72	LT	4	39	0.0027	0.08	WATER STREET SW CURB RAMP
640+40 640+70	RT	4	40	0.0028	0.08	WATER STREET SE CURB RAMP
641+18 641+45	LT	4	38	0.0026	0.08	WATER STREET NW CURB RAMP
641+06 641+28	RT	10	91	0.0064	0.18	WATER STREET NE CURB RAMP
644+83 645+11	RT	11	101	0.0071	0.20	ISLAND DRIVE SE CURB RAMP
645+48 645+73	RT	9	78	0.0055	0.16	ISLAND DRIVE NE CURB RAMP
647+24 647+49	LT	8	70	0.0049	0.14	SUNSET DRIVE SW CURB RAMP
647+80 647+92	LT	3	22	0.0016	0.04	SUNSET DRIVE NW CURB RAMP
669+63 669+92	RT	7	60	0.0042	0.12	STH 22/23 SE CURB RAMP
669+13 669+57	LT	1	13	0.0009	0.03	STH 22/23 NE CURB RAMP
678+64 678+79	LT	9	85	0.0060	0.17	CASS STREET NW CURB RAMP
679+25 679+33	LT	4	38	0.0027	0.08	CASS STREET NE CURB RAMP
678+64 678+80	RT	7	59	0.0041	0.12	CASS STREET SW CURB RAMP
679+18 679+33	RT	4	39	0.0027	0.08	CASS STREET SE CURB RAMP
686+63 686+91	LT	27	240	0.0168	0.48	EAST STREET NW CURB RAMP
687+31 687+51	LT	6	55	0.0038	0.11	EAST STREET NE CURB RAMP
687+37 687+48	RT	4	38	0.0027	0.08	EAST STREET SE CURB RAMP
690+02 690+69	LT	3	30	0.0021	0.06	CHURCH STREET NW CURB RAMP
10"N"+32 10"N"+54	LT	5	41	0.0029	0.07	CHURCH STREET NE CURB RAMP
10"N"+43 10"N"+54	RT	4	34	0.0024	0.07	CHURCH STREET SE CURB RAMP
PROJECT TOTALS:		252	2261	0.16	5	

INLET PROTECTION

STATION	LOCATION	628.7015 TYPE C EACH
620+86	RT	1
621+59	RT	1
623+78	RT	1
625+13	LT	1
625+12	RT	1
627+60	RT	1
627+77	LT	1
9"C"+44	LT	1
9"C"+44	RT	1
629+91	RT	1
629+91	LT	1
631+94	RT	1
631+94	LT	1
9"D"+29	RT	1
9"D"+29	LT	1
634+15	RT	1
634+15	LT	1
636+08	RT	1
636+08	LT	1
9"E"+53	RT	1
9"E"+53	LT	1
640+32	LT	1
640+40	RT	1
9"F"+47	RT	1
9"F"+47	LT	1
642+83	LT	1
642+89	RT	1
643+01	RT	1
643+01	LT	1
644+87	RT	1
645+20	LT	1
645+80	RT	1
650+40	RT	1
650+40	LT	1
652+82	RT	1
653+00	LT	1
654+18	RT	1
654+18	LT	1
654+66	LT	1
654+70	RT	1
656+58	LT	1

INLET PROTECTION

STATION	LOCATION	628.7015 TYPE C EACH
656+60	RT	1
660+66	RT	1
661+00	RT	1
661+00	LT	1
661+72	RT	1
661+72	LT	1
664+23	RT	1
664+45	LT	1
667+15	RT	1
9"K"+31	RT	1
9"K"+31	LT	1
670+20	LT	1
670+55	RT	1
674+75	RT	1
675+00	LT	1
676+80	LT	1
676+85	RT	1
678+90	RT	1
678+90	LT	1
10"L"+32	LT	1
10"L"+32	RT	1
679+25	LT	1
682+75	RT	1
682+75	LT	1
686+90	RT	1
686+81	LT	1
687+38	LT	1
690+60	LT	1
10"N"+37	LT	1
10"N"+44	RT	1
12"N"+69	RT	1
12"N"+70	LT	1
15"N"+13	RT	1
15"N"+14	LT	1

PROJECT TOTAL: 75

continued...

SIGN LISTING

SIGN NO.	SIGN CODE	MESSAGE	STATION	LOCATION	SIZE	POSTS WOOD	637.2210	637.2230	638.2102	638.2602	638.3000	REMARKS
						4X6-INCH 634.0618 18-FT EACH	SIGNS TYPE II REFLECTIVE H SF	SIGNS TYPE II REFLECTIVE F SF	MOVING SIGNS TYPE II EA	REMOVING SIGNS TYPE II EA	REMOVING SMALL SIGN SUPPORTS EA	
1-01	R2-1	SPEED LIMIT 35	618+75	RT	24"X30"	1	5.00	---	---	1	1	
1-02	I2-3	MONTELLO POPULATION 1495	620+32	RT	24"X60"	2	10.00	---	---	1	2	
1-03	R1-1	STOP	621+56	RT	30"X30"	1	5.18	---	---	1	1	INSTALL ON E 5TH STREET
1-04		5TH ST, MAIN ST	621+58	RT	---	---	---	---	1	---	---	RE-INSTALL ON 5TH STREET AFTER CURB RAMP IS CONSTRUCTED
1-05	R2-1	SPEED LIMIT 55	618+75	LT	24"X30"	1	5.00	---	---	1	1	
1-06	I2-2	PARDEEVILLE 19	620+32	LT	15"X84"	2	8.75	---	---	1	2	
1-07	J4-1	SOUTH, 22	622+00	LT	24"X36"	1	6.00	---	---	1	1	
2-01	J2-2	F, [LEFT ARROW], NORTH, 22, [UP ARROW]	623+75	RT	---	---	---	---	---	1	1	
2-02	J13-1	F, LA	623+75	RT	24"X45"	1	7.50	---	---	---	---	
2-03	W11-2	PEDESTRIAN CROSSING SYMBOL	625+25	RT	30"X30"	1	---	6	---	---	---	
2-04	R2-1	SPEED LIMIT 35	625+50	RT	24"X30"	1	5	---	---	1	1	
2-05	J13-1	K, [LEFT ARROW]	627+50	RT	24"X45"	1	7.50	---	---	1	1	
2-06	R2-1	SPEED LIMIT 25	629+50	RT	24"X30"	1	5	---	---	1	1	
2-07	---	ARROW	623+75	LT	---	---	---	---	---	1	1	
2-08	R1-1	STOP	624+00	LT	30"X30"	1	5.18	---	---	1	1	INSTALL ON CTH F
2-09	J13-1	F, [RIGHT ARROW]	625+00	LT	24"X45"	1	7.50	---	---	1	1	
2-10	W11-2	PEDESTRIAN CROSSING SYMBOL	625+25	RT	30"X30"	1	---	6	---	---	---	
2-11	---	NO PARKING ANY TIME	627+50	LT	---	---	---	---	---	1	1	
2-12	J1-1	JCT, F	627+90	LT	24"X39"	1	7	---	---	---	---	
2-13	R1-1	STOP	628+00	LT	30"X30"	1	5.18	---	---	1	1	INSTALL ON 4TH STREET
2-14	---	4TH ST, MAIN ST	628+50	LT	---	---	---	---	1	---	---	RE-INSTALL ON 4TH STREET AFTER CURB RAMP IS CONSTRUCTED
2-15	J13-1	K, [RIGHT ARROW]	628+60	LT	24"X45"	1	7.50	---	---	1	1	
3-01	W11-2	PEDESTRIAN CROSSING SYMBOL	636+20	RT	---	---	---	---	---	1	1	
3-02	R1-1	STOP	636+80	RT	30"X30"	1	5.18	---	---	1	1	INSTALL ON 2ND STREET (WEST)
3-03	W11-2	PEDESTRIAN CROSSING SYMBOL	637+00	RT	30"X30"	1	---	6	---	---	---	
3-03	W16-7L	LEFT DIAGONAL DOWNWARD POINTING ARROW (YELLOW)	637+00	RT	24"X12"	---	---	2	---	---	---	
3-04	R1-1	STOP	632+20	LT	30"X30"	1	5.18	---	---	1	1	INSTALL ON 3RD STREET
3-05	---	3RD ST, MAIN ST	632+75	LT	---	---	---	---	1	---	---	RE-INSTALL ON 3RD STREET AFTER CURB RAMP IS CONSTRUCTED
3-06	---	JCT, K, F	632+90	LT	---	---	---	---	---	1	1	
3-07	R1-1	STOP	636+30	LT	30"X30"	1	5.18	---	---	1	1	INSTALL ON 2ND STREET (EAST)
3-08	---	2ND ST, MAIN ST	637+00	LT	---	---	---	---	1	---	---	RE-INSTALL ON 2ND STREET (EAST) AFTER CURB RAMP IS CONSTRUCTED
3-09	W11-2	PEDESTRIAN CROSSING SYMBOL	637+20	LT	30"X30"	1	---	6	---	1	1	
3-09	W16-7L	LEFT DIAGONAL DOWNWARD POINTING ARROW (YELLOW)	637+20	LT	24"X12"	---	---	2	---	---	---	
4-01	R1-1	STOP	641+05	RT	30"X30"	1	5.18	---	---	1	1	INSTALL ON WATER STREET (WEST) AFTER CURB RAMP IS CONSTRUCTED
4-02	R2-1	SPEED LIMIT 25	641+50	RT	24"X30"	1	5	---	---	1	1	
4-03	I3-1	FOX RIVER	642+75	RT	24"X36"	1	6	---	---	1	1	
4-04	---	ISLAND DR, MAIN ST	645+00	RT	---	---	---	---	1	---	---	RE-INSTALL ON ISLAND DRIVE AFTER CURB RAMP IS CONSTRUCTED
4-05	R1-1	STOP	641+05	RT	30"X30"	1	5.18	---	---	1	1	INSTALL ON ISLAND DRIVE
4-06	I3-1	FOX RIVER LOCK	646+50	RT	---	---	---	---	---	1	1	
4-07	J1-1	JCT, K	638+50	LT	24"X39"	1	7	---	---	---	---	
4-08	R2-1	SPEED LIMIT 25	639+60	LT	24"X30"	1	5	---	---	1	1	
4-09	R1-1	STOP	640+75	LT	30"X30"	1	5.18	---	---	1	1	INSTALL ON WATER STREET (EAST) AFTER CURB RAMP IS CONSTRUCTED

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SIGN LISTING

SIGN NO.	SIGN CODE	MESSAGE	STATION	LOCATION	SIZE	POSTS WOOD	637.2210	637.2230	638.2102	638.2602	638.3000	REMARKS
						4X6-INCH 634.0618	SIGNS TYPE II	SIGNS TYPE II	MOVING SIGNS TYPE II	REMOVING SIGNS TYPE II	REMOVING SMALL SIGN SUPPORTS	
						18-FT EACH	REFLECTIVE H SF	REFLECTIVE F SF	EA	EA	EA	
4-10	---	WATER ST, MAIN ST	641+25	LT	---	---	---	---	1	---	---	RE-INSTALL ON WATER STREET (EAST) AFTER CURB RAMP IS CONSTRUCTED
4-11	I3-1	FOX RIVER	644+75	LT	24"X36"	1	6	---	---	1	1	
4-12	W11-2	PEDESTRIAN CROSSING SYMBOL	645+50	LT	---	---	---	---	---	1	1	
5-01	R2-1	SPEED LIMIT 25	648+25	RT	24"X30"	1	5	---	---	1	1	
5-02	I3-1	MONTELLO RIVER	650+10	RT	24"X54"	1	9	---	---	1	1	
5-03	R2-1	SPEED LIMIT 25	652+00	RT	---	---	---	---	---	1	---	REMOVE FROM EXISTING POLE
5-04	I3-1	FOX RIVER LOCK	647+50	LT	---	---	---	---	---	1	1	
5-05	---	SUNSET DR, MAIN ST	647+95	LT	---	---	---	---	1	---	---	RE-INSTALL ON SUNSET DRIVE AFTER CURB RAMP IS CONSTRUCTED
5-06	I3-1	MONTELLO RIVER	651+90	LT	24"X54"	1	9	---	---	1	1	
5-07	R2-1	SPEED LIMIT 25	653+50	LT	24"X30"	1	5	---	---	1	1	
6-01	R2-1	SPEED LIMIT 25	659+50	RT	24"X30"	1	5	---	---	1	1	
6-02	J2-2	WEST, 23, [AHEAD LEFT ARROW], EAST, 23, [AHEAD RIGHT ARROW]	662+75	RT	48"X57"	1	19	---	---	1	1	
6-03	J4-1	SOUTH, 22	662+50	LT	24"X36"	1	6	---	---	1	1	
7-01	W3-3	SIGNAL AHEAD SYMBOL	663+50	RT	36"X36"	1	---	9	---	1	1	
7-02	J2-1	NORTH, 22, [AHEAD RIGHT ARROW]	664+20	RT	24"X57"	1	10	---	---	1	1	
7-03	D1-3	OXFORD, PRINCETON, WAUTOMA	665+30	RT	36"X72"	2	18	---	---	1	2	
7-04	R4-7	KEEP RIGHT SYMBOL	667+20	CL	---	---	---	---	---	1	1	REMOVE FROM EXISTING MEDIAN
7-05	R4-7	KEEP RIGHT SYMBOL	667+50	CL	24"X30"	1	5	---	---	---	---	INSTALL ON NEW MEDIAN
7-05	R3-2	NO LEFT TURN	667+50	CL	24"X24"	---	4	---	---	---	---	
7-06	W12-1D	DOUBLE DOWN ARROWS	668+50	CL	24"X24"	1	---	4	---	1	1	INSTALL ON ISLAND
7-07	R1-2	YIELD	669+75	RT	36"X31"	1	4	---	---	1	1	
7-08	R4-7	KEEP RIGHT SYMBOL	668+40	CL	24"X30"	1	5	---	---	---	---	INSTALL ON NEW MEDIAN
7-09	R1-1F	FOLDING STOP	9"K"+40	RT	30"X30"	1	5	---	---	1	1	INSTALL ON SIGNAL POLE
7-10	J2-2	EAST, 23 SOUTH, 22, [RIGHT ARROW]	9"K"+25	RT	48"X57"	1	19	---	---	1	1	
7-11	R4-7	KEEP RIGHT SYMBOL	668+50	CL	---	---	---	---	---	1	1	REMOVE FROM EXISTING MEDIAN
7-12	J3-3	WEST, 23, [LEFT ARROW], NORTH, 22, [RIGHT ARROW], EAST, 23, [RIGHT ARROW]	669+00	LT	72"X57"	2	29	---	---	1	2	
7-13	J2-2	SOUTH, 22, [LEFT ARROW] WEST, 23	669+05	LT	48"X57"	1	19	---	---	1	1	
7-14	R1-1F	FOLDING STOP	669+50	LT	30"X30"	1	5	---	---	1	1	INSTALL ON SIGNAL POLE
7-15	R1-1F	FOLDING STOP	668+60	CL	30"X30"	1	5	---	---	1	1	INSTALL ON ISLAND SIGNAL POLE
7-16	R3-8E	LEFT ARROW ONLY, UP ARROW ONLY	670+78	LT	36"X30"	1	8	---	---	1	1	
7-17	---	NO PARKING ANY TIME	672+00	LT	---	---	---	---	1	---	---	INSTALL ON SAME POST AS 7-18
7-18	W3-3	SIGNAL AHEAD SYMBOL	672+00	LT	36"X36"	1	---	9	---	1	1	
8-01	R2-1	SPEED LIMIT 25	672+75	LT	24"X30"	1	5	---	---	1	1	
8-02	J2-2	SOUTH, 22, [AHEAD LEFT ARROW] WEST, 23, [UP ARROW]	674+10	LT	48"X57"	1	19	---	---	1	1	
8-03	R1-1	STOP	678+95	LT	30"X30"	1	5.18	---	---	1	1	INSTALL ON CASS STREET
8-03	R5-1	DO NOT ENTER	678+95	LT	30"X30"	---	6	---	---	---	---	INSTALL ON CASS STREET
8-04	R2-1	SPEED LIMIT 25	680+25	RT	24"X30"	1	5	---	---	1	1	
9-01	S1-1	SCHOOL ADVANCE	684+50	RT	36"X36"	1	---	7	---	1	1	
9-01	S16-9P	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	684+50	RT	30"X18"	---	---	4	---	---	---	
9-02	S4-51	SCHOOL SPEED LIMIT	685+50	RT	24"X48"	1	---	8	---	1	1	
9-03	S1-1	SCHOOL ADVANCE	687+50	RT	36"X36"	1	---	7	---	1	1	
9-03	S16-7L	LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT YELLOW)	687+50	RT	30"X18"	---	---	4	---	---	---	

cont. on next page...

SIGN LISTING

SIGN NO.	SIGN CODE	MESSAGE	STATION	LOCATION	SIZE	POSTS WOOD	637.2210	637.2230	638.2102	638.2602	638.3000	REMARKS	
						4X6-INCH 634.0618 18-FT EACH	SIGNS TYPE II REFLECTIVE H SF	SIGNS TYPE II REFLECTIVE F SF	MOVING SIGNS TYPE II EA	REMOVING SIGNS TYPE II EA	REMOVING SMALL SIGN SUPPORTS EA		
9-04	D1-2	PRINCETON, WAUTOMA	687+75	RT	30"X72"	1	15				1		
9-05	J2-2	NORTH, 22, [AHEAD LEFT ARROW], EAST, 23 [UP ARROW]	688+75	RT	48"X57"	1	19	---	---		1		
9-06	R2-1	SPEED LIMIT 25	686+50	LT	24"X30"	1	5	---	---		1		
9-07	R1-1	STOP	686+75	LT	30"X30"	1	5.18	---	---		1	INSTALL ON EAST STREET	
9-08	S1-1	SCHOOL ADVANCE	687+50	LT	36"X36"	1	---	7	---		1		
9-08	S16-7L	LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT YELLOW)	687+50	LT	30"X18"	---	---	4	---		---		
9-09	J1-2	SOUTH, 22, WEST, 23	688+20	LT	48"X39"	1	13	---	---		---		
10-01	J2-2	NORTH, 22, [LEFT ARROW] EAST, 23	690+20	RT	48"X57"	1	19	---	---		1		
10-02	J3-3	EAST, 23, [LEFT ARROW], SOUTH, 22, [RIGHT ARROW], WEST, 23, [RIGHT ARROW]	690+40	RT	72"X57"	2	29	---	---		1	2	
10-03	S1-1 S16-7L	SCHOOL ADVANCE, LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT YELLOW)	10"N"+20	RT	---	---	---	---	---		1	---	REMOVE FROM EXISTING POLE
10-04	S1-1	SCHOOL ADVANCE	10"N"+40	RT	36"X36"	1	---	7	---		---		
10-04	S16-7L	LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT YELLOW)	10"N"+40	RT	30"X18"	---	---	4	---		---		
10-05	J1-1	EAST, 23	11"N"+00	RT	24"X39"	1	7	---	---		1	1	
10-06	W1-2L	LEFT CURVE	13"N"+25	RT	30"X30"	1	---	6	---		1	1	
10-07	J12-1	C, [AHEAD RIGHT ARROW]	14"N"+50	RT	---	---	---	---	---		1	1	
10-08	---	SOUTH, 22, WEST, 23	689+50	LT	---	---	---	---	---		1	1	
10-09	---	MONTELLO ST, CHURCH ST	690+00	LT	---	---	---	---	1		---	---	RE-INSTALL ON CHURCH STREET AFTER CURB RAMP IS CONSTRUCTED
10-10	R1-1	STOP	690+05	LT	30"X30"	1	5.18	---	---		1	1	INSTALL ON CHURCH STREET
10-11	J3-2	SOUTH, 22, NORTH, 22, [RIGHT ARROW]	11"N"+40	LT	48"X57"	1	19	---	---		1	1	
10-12	S1-1	SCHOOL ADVANCE	11"N"+75	LT	36"X36"	1	---	7	---		1	1	
10-13	D1-2	PARDEEVILLE, WIS DELLS, WAUTOMA	12"N"+90	LT	36"X78"	1	19.50	---	---		1	1	
10-14	S1-1	SCHOOL ADVANCE	14"N"+50	LT	36"X36"	1	---	7	---		1	1	
10-14	S16-9P	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	14"N"+50	LT	30"X18"	---	---	4	---		---	---	
10-15	J3-3	SOUTH, 22, [UP ARROW], WEST, 23, [UP ARROW], NORTH, 22, [AHEAD RIGHT ARROW]	16"N"+55	LT	72"X57"	2	29	---	---		1	2	
S126	---	EAST, 23, [UP ARROW], SOUTH, 22, [RIGHT ARROW]	---	RT	---	---	---	---	1		---	---	STAGE ONE TRAFFIC CONTROL
S311	---	EAST, 23, [UP ARROW], SOUTH, 22, [RIGHT ARROW]	---	RT	---	---	---	---	1		---	---	STAGE THREE TRAFFIC CONTROL
S326	---	SOUTH, 22, [LEFT ARROW] WEST, 23, [UP ARROW]	---	LT	---	---	---	---	1		---	---	STAGE THREE TRAFFIC CONTROL
PROJECT TOTALS:						82	573	123	12	80	84		

FIELD OFFICE

PROJECT	642.5201 TYPE C EACH
6054-05-75	1
PROJECT TOTAL:	1

TRAFFIC CONTROL

(SEE TRAFFIC CONTROL SCHEDULE SHEETS FOR LOCATION AND QUANTITY BREAKDOWN)

PROJECT	603.8000 CONCRETE BARRIER TEMPORARY PRECAST DELIVERED LF	603.8125 CONCRETE BARRIER TEMPORARY PRECAST INSTALLED LF	643.0300 DRUMS DAYS	643.0410 BARRICADES TYPE II DAYS	643.0420 BARRICADES TYPE III DAYS	643.0705 WARNING LIGHTS TYPE A DAYS	643.0715 WARNING LIGHTS TYPE C DAYS	643.0900 SIGNS DAYS	643.0920 COVERING TYPE II EACH	643.5000 TRAFFIC CONTROL EACH	646.9000 MARKING LINE 4-INCH LF	646.9300 MARKING SPECIAL MARKING EACH	649.0150 TEMPORARY MARKING LINE 4-INCH LF	649.0250 TEMPORARY MARKING LINE 8-INCH LF	649.0550 TEMPORARY MARKING ARROW EACH	649.0650 TEMPORARY MARKING WORD EACH	649.0850 TEMPORARY MARKING STOP LINE 18-INCH LF	SPV.0105.01 TEMPORARY TRAFFIC SIGNALS FOR INTERSECTION LS	SPV.0105.02 TEMPORARY VEHICLE DETECTION STH 22/23 INTERSECTION LS
6054-05-75	100	200	2588	1344	1794	814	1858	7141	6	0.50	500	2	980	290	4	2	88	1	1
PROJECT TOTALS:	100	200	2588	1344	1794	814	1858	7141	6	0.50	500	2	980	290	4	2	88	1	1

MARKING

STATION - STATION	LOCATION	646.1020		646.3020		646.4520		646.5020		646.5120		646.6120		646.7420		646.8020		646.8220		649.0105		REMARKS
		LINE 4-INCH		LINE 8-INCH	LINE SAME DAY	ARROW		WORD ONLY	STOP LINE 18-INCH	LINE 6-INCH	TRANSVERSE	CORRUGATED	CURB	ISLAND	NOSE	MARKING LINE PAINT 4-INCH						
		LF	LF	LF	LF	EA	EA	EA	LF	LF	SF	LF	EA	EA	LF							
614+50 - 624+00	LT & RT	1,510	---	100	1,900	---	---	---	---	---	---	---	---	---	---	---	---	---	---	1,900		
624+75 - 625+16	LT & RT	---	---	---	82	---	---	---	---	---	---	---	---	---	---	---	---	---	---	82		
625+22 - 627+80	LT & RT	---	---	---	516	---	---	---	---	---	---	---	---	---	---	---	---	---	---	516		
628+72 - 632+50	CL	---	---	---	756	---	---	---	---	---	---	---	---	---	---	---	---	---	---	756		
632+50 - 636+88	LT & RT	---	---	---	876	---	---	---	---	---	---	---	---	---	---	---	---	---	---	876		
636+94 - 641+00	LT & RT	---	---	---	812	---	---	---	---	---	---	---	---	---	---	---	---	---	---	812		
641+00 - 645+25	CL	---	---	---	850	---	---	---	---	---	---	---	---	---	---	---	---	---	---	850		
645+25 - 647+75	CL	---	---	---	500	---	---	---	---	---	---	---	---	---	---	---	---	---	---	500		
647+25 - 662+00	CL	---	---	---	2,950	---	---	---	---	---	---	---	---	---	---	---	---	---	---	2,950		
662+00 - 668+48	LT & RT	---	1,406	95	---	---	---	---	---	37	158	17	44	3	---	---	---	---	---	---	CURB EPOXY ON MEDIAN ONLY	
669+40 - 670+99	LT & RT	---	296	150	---	1	2	1	22	70	---	---	---	---	---	---	---	---	---	---		
670+99 - 678+68	LT & RT	---	---	---	1,538	---	---	---	---	---	40	---	---	---	---	---	---	---	---	1,538		
678+74 - 679+25	LT & RT	---	---	---	102	---	---	---	---	---	88	---	---	---	---	---	---	---	---	102		
679+31 - 687+39	LT & RT	---	---	---	1,616	---	---	---	---	---	81	---	---	---	---	---	---	---	---	1,616		
687+45 - 690+06	LT & RT	---	---	---	522	---	---	---	---	---	40	---	---	---	---	---	---	---	---	522		
690+61 - 690+82	LT & RT	---	32	---	---	---	---	---	---	15	89	---	---	---	---	---	---	---	---	---		
10'A'+43	LT	---	---	---	---	---	---	---	---	27	---	---	---	---	---	---	---	---	---	---	E 5TH STREET	
7'B'+69 - 9'B'+45	RT	---	352	---	---	---	---	---	---	27	---	---	---	---	---	---	---	---	---	---	CTH F	
8'C'+00 - 9'C'+56	RT	---	312	---	---	---	---	---	---	18	---	---	---	---	---	---	---	---	---	---	4TH STREET	
9'C'+60 - 9'C'+65	LT & RT	---	---	---	---	---	---	---	---	---	85	---	---	---	---	---	---	---	---	---	4TH STREET	
9'D'+61 - 9'D'+71	LT & RT	---	---	---	---	---	---	---	---	17	84	---	---	---	---	---	---	---	---	---	3RD STREET	
9'E'+62 - 9'E'+72	LT & RT	---	---	---	---	---	---	---	---	17	84	---	---	---	---	---	---	---	---	---	2ND STREET	
10'E'+30 - 10'E'+40	LT & RT	---	---	---	---	---	---	---	---	15	83	---	---	---	---	---	---	---	---	---	2ND STREET	
9'F'+61 - 6'F'+71	LT & RT	---	---	---	---	---	---	---	---	18	88	---	---	---	---	---	---	---	---	---	WATER STREET	
10'F'+30 - 10'F'+40	LT & RT	---	---	---	---	---	---	---	---	13	71	---	---	---	---	---	---	---	---	---	WATER STREET	
10'G'+29 - 10'G'+39	LT & RT	---	---	---	---	---	---	---	---	14	73	---	---	---	---	---	---	---	---	---	ISLAND DRIVE	
9'H'+56 - 9'H'+57	LT & RT	---	---	---	---	---	---	---	---	16	53	---	---	---	---	---	---	---	---	---	SUNSET DRIVE	
8'K'+65 - 9'K'+57	LT & RT	---	124	---	---	---	---	---	---	12	64	---	---	---	---	---	---	---	---	---	STH 23	
10'L'+25 - 10'L'+34	LT & RT	---	---	---	---	---	---	---	---	11	42	---	---	---	---	---	---	---	---	---	CASS STREET	
10'M'+27 - 10'M'+37	LT & RT	---	---	---	---	---	---	---	---	17	77	---	---	---	---	---	---	---	---	---	EAST STREET	
10'N'+45 - 16'N'+58	LT & RT	---	---	---	1,214	---	---	---	---	---	80	---	---	---	---	---	---	---	---	---	1,214	STH 23
PROJECT TOTALS:		1,510	2,522	345	14,234	3	1	296	1,631	17	44	3	14,234									

NOTE: ALL MARKING LINE EPOXY 4-INCH YELLOW IS SOLID, NO PASSING.
DO NOT REINSTALL WHITE SKIPS ON STH 23 FROM CTH F TO CTH K.

CONSTRUCTION STAKING

	650.5500	650.8000	650.9000	650.9910
	CURB GUTTER AND	RESURFACING	CURB	SUPPLEMENTAL
	CURB & GUTTER	REFERENCE	RAMPS	CONTROL
STATION - STATION	LF	LF	EACH	01. 6054-05-75 LS
614+50 - 690+81	942	7630	38	1
PROJECT TOTALS:	942	7630	38	1

CONCRETE BASE

		654.0101
		TYPE 1
STATION	LOCATION	EACH
668+42	LT	1
PROJECT TOTAL:		1

PEDESTAL BASE

		657.0100
		TYPE 1
STATION	LOCATION	EACH
668+42	LT	1
PROJECT TOTAL:		1

TRAFFIC SIGNAL STANDARDS ALUMINUM

		657.0430
		10-FOOT
STATION	LOCATION	EACH
668+42	LT	1
PROJECT TOTAL:		1

PEDESTRIAN SIGNAL FACE

		658.0416
		16-INCH
STATION	LOCATION	EACH
668+42	LT	1
PROJECT TOTAL:		1

PEDESTRIAN PUSH BUTTON

		658.0500
		EACH
STATION	LOCATION	
668+42	LT	1
PROJECT TOTAL:		1

SAWING

690.0150 690.0250
ASPHALT CONCRETE

STATION - STATION	LOCATION	LF	LF	REMARKS
614+50 - 614+50	LT & RT	34	---	
621+55 - 621+93	RT	29	10	
624+56 - 624+83	LT	24	10	
625+15 - 625+27	LT & RT	28	30	
627+80 - 628+08	LT	20	10	
628+43 - 628+71	LT	20	10	
631+96 - 632+25	LT	20	10	
632+58 - 632+83	LT	20	10	
636+03 - 634+40	RT	19	10	
636+12 - 636+41	LT	19	10	
636+75 - 637+04	LT	31	10	
636+75 - 637+04	RT	32	10	
640+40 - 640+74	RT	20	10	
640+43 - 640+77	LT	20	26	
641+01 - 641+28	RT	20	10	
641+13 - 628+45	LT	20	10	
644+83 - 645+16	RT	22	10	
645+43 - 645+73	RT	20	10	
647+24 - 647+52	LT	15	11	
647+76 - 647+93	LT	15	10	
666+94 - 669+46	LT & RT	---	805	STAGE 1
667+16 - 669+91	RT	---	698	STAGE 2
9"K"+52 - 670+34	LT	---	363	STAGE 3
622+74 - 622+80	RT	---	36	STAGE 5
664+40 - 666+62	LT	---	254	STAGE 4
622+74 - 622+80	RT	---	36	STAGE 5
678+64 - 10"L"+43	LT	---	79	
10"L"+48 - 679+33	LT	---	55	
678+64 - 678+78	RT	---	30	
679+22 - 679+35	RT	---	30	
686+80 - 10"M"+36	LT	---	40	
10"M"+36 - 687+50	LT	---	53	
687+34 - 687+48	RT	---	29	
690+12 - 690+69	LT	---	41	
690+70 - 10"N"+55	LT	---	56	
10"N"+39 - 10"N"+55	RT	---	31	
11"A"+50	LT & RT	30	---	
7"B"+70	LT & RT	24	---	
8"C"+00	LT & RT	24	---	
9"D"+20	LT & RT	30	---	
9"E"+00	LT & RT	30	---	
10"E"+55	LT & RT	30	---	
9"F"+35	LT & RT	32	---	
10"F"+50	LT & RT	24	---	
10"G"+55	LT & RT	26	---	
9"H"+55	LT & RT	24	---	
10"L"+35	LT & RT	22	---	
10"M"+35	LT & RT	36	---	
690+81	LT & RT	38	---	
16"N"+57	LT & RT	44	---	
PROJECT TOTALS:		862	2,863	

REMOVE AND SALVAGE PEDESTRIAN SIGNAL HEAD

STATION - STATION	LOCATION	SPV.0060.02 EACH	REMARKS
9"K"+46	16' RT	1	REMOVE FROM EXISTING SIGNAL POLE
PROJECT TOTAL:		1	

REMOVE PEDESTRIAN PUSH BUTTON

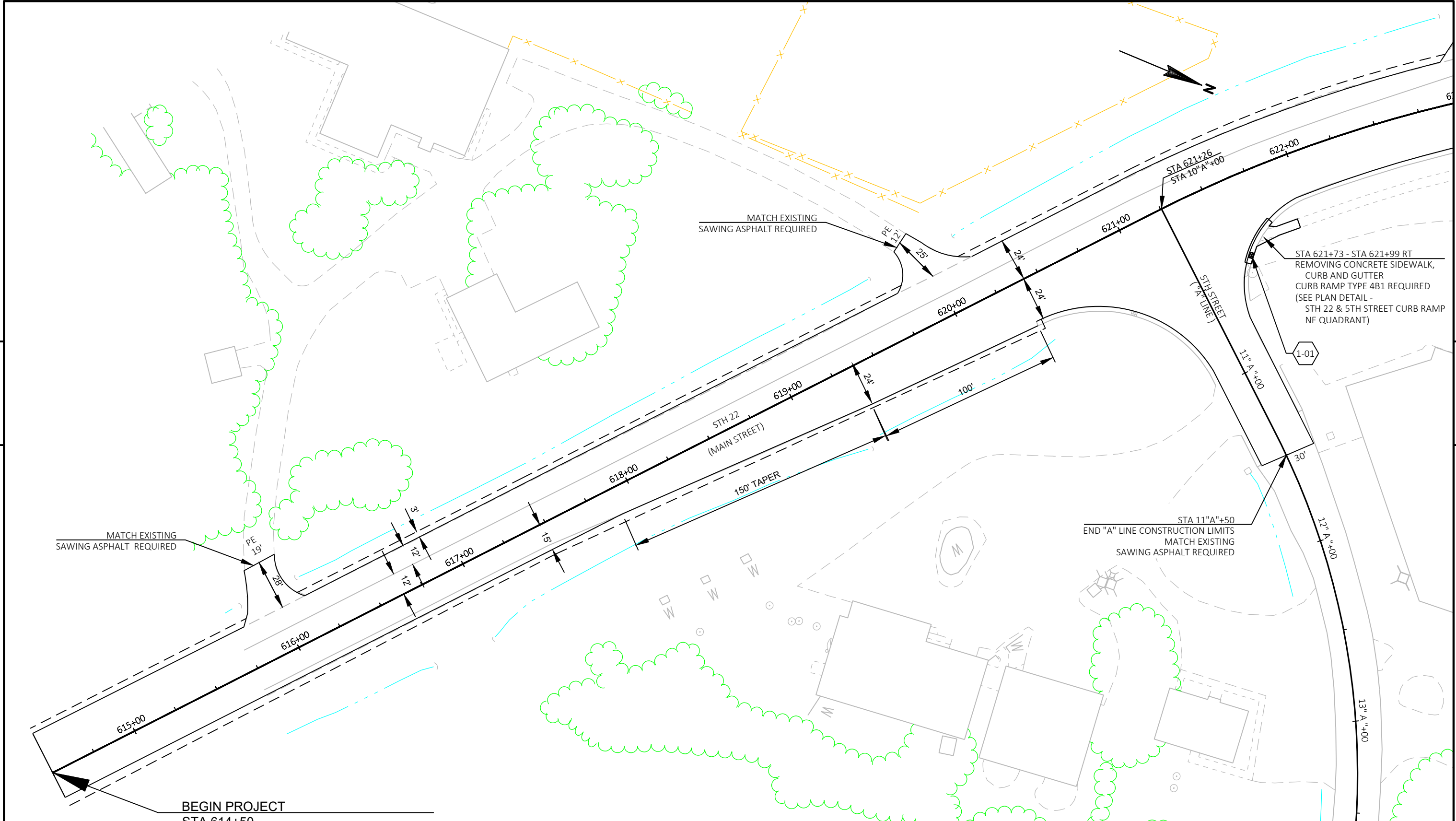
STATION - STATION	LOCATION	SPV.0060.03 EACH	REMARKS
9"K"+46	16' RT	1	REMOVE FROM EXISTING SIGNAL POLE
PROJECT TOTAL:		1	

CRUSHED RED GRANITE MULCH

STATION - STATION	LOCATION	SPV.0185.01 SY	REMARKS
636+12 - 636+39	40' LT	7	STH 22 AND 2ND STREET SW QUADRANT
636+77 - 637+05	40' LT	6	STH 22 AND 2ND STREET NW QUADRANT
640+39 - 640+71	40' RT	7	STH 22 AND WATER STREET SE QUADRANT
641+15 - 641+46	40' LT	7	STH 22 AND WATER STREET NW QUADRANT
PROJECT TOTAL:		27	

5

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MATCH EXISTING
SAWING ASPHALT REQUIRED

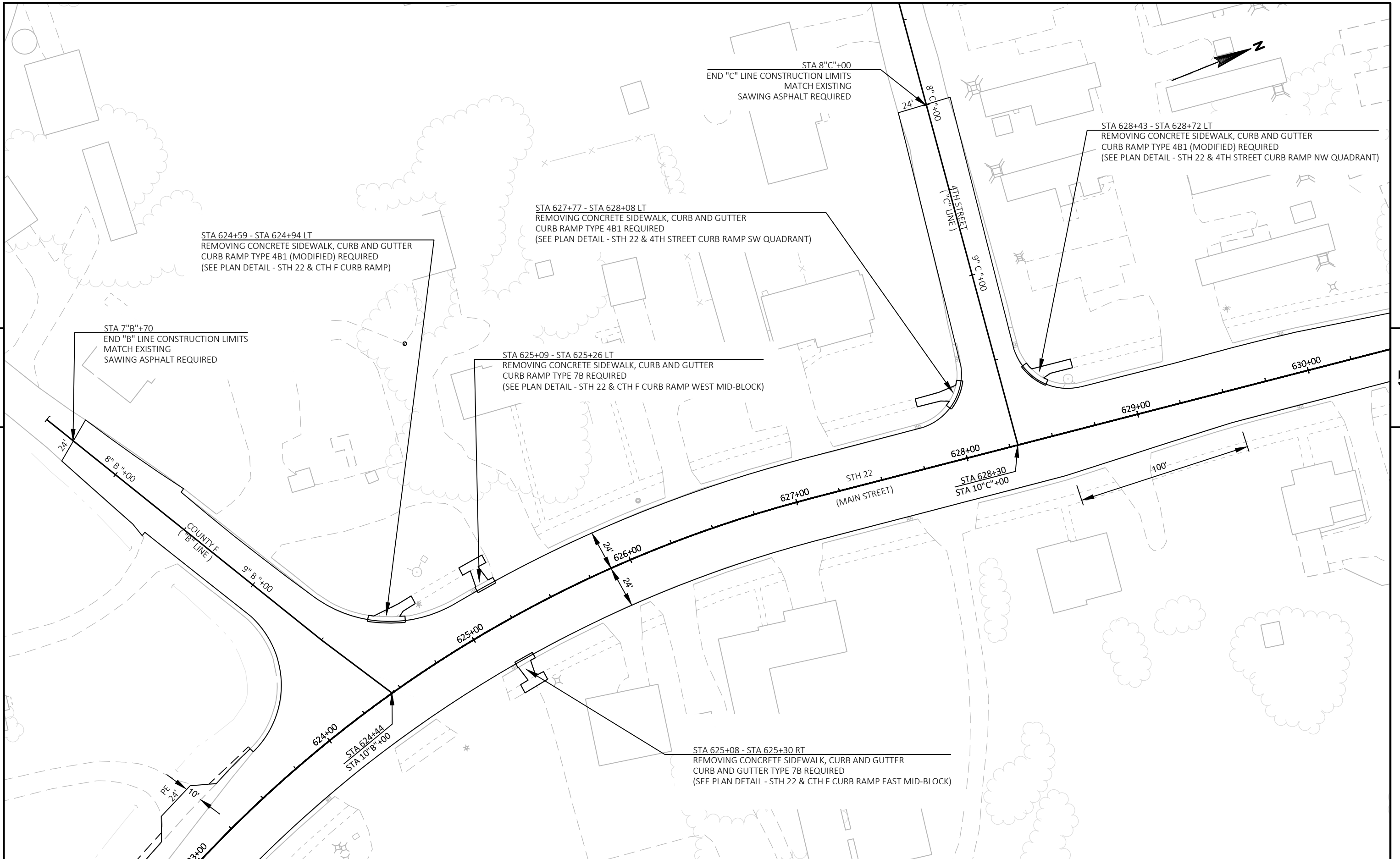
MATCH EXISTING
SAWING ASPHALT REQUIRED

STA 621+73 - STA 621+99 RT
REMOVING CONCRETE SIDEWALK,
CURB AND GUTTER
CURB RAMP TYPE 4B1 REQUIRED
(SEE PLAN DETAIL -
STH 22 & 5TH STREET CURB RAMP
NE QUADRANT)

STA 11\"A\"+50
END \"A\" LINE CONSTRUCTION LIMITS
MATCH EXISTING
SAWING ASPHALT REQUIRED

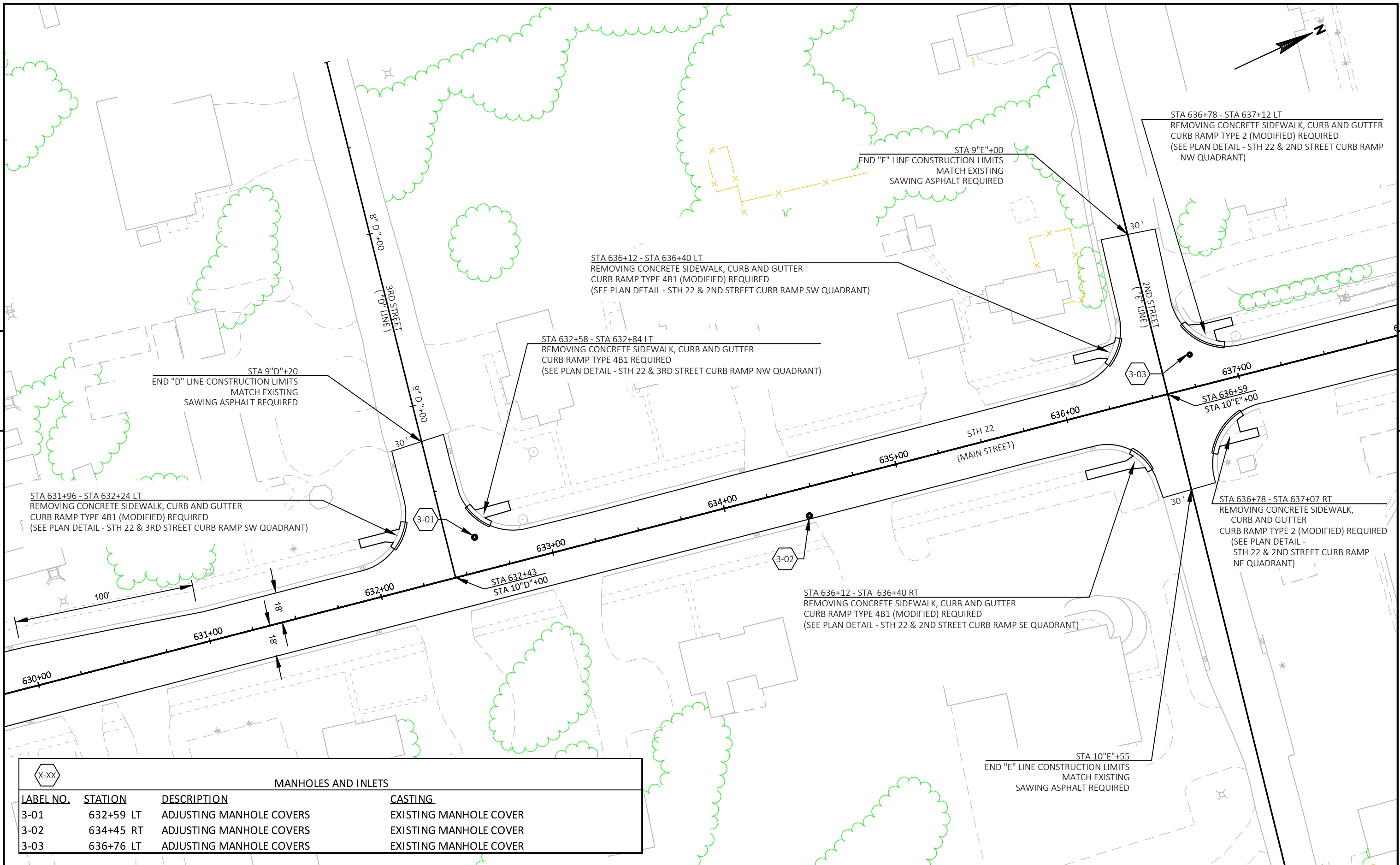
BEGIN PROJECT
STA 614+50
BEGIN ASPHALTIC MILLING AND PAVING
MATCH EXISTING
SAWING ASPHALT REQUIRED

MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
1-01	621+59 RT	ADJUSTING INLET COVERS	EXISTING INLET COVER



5

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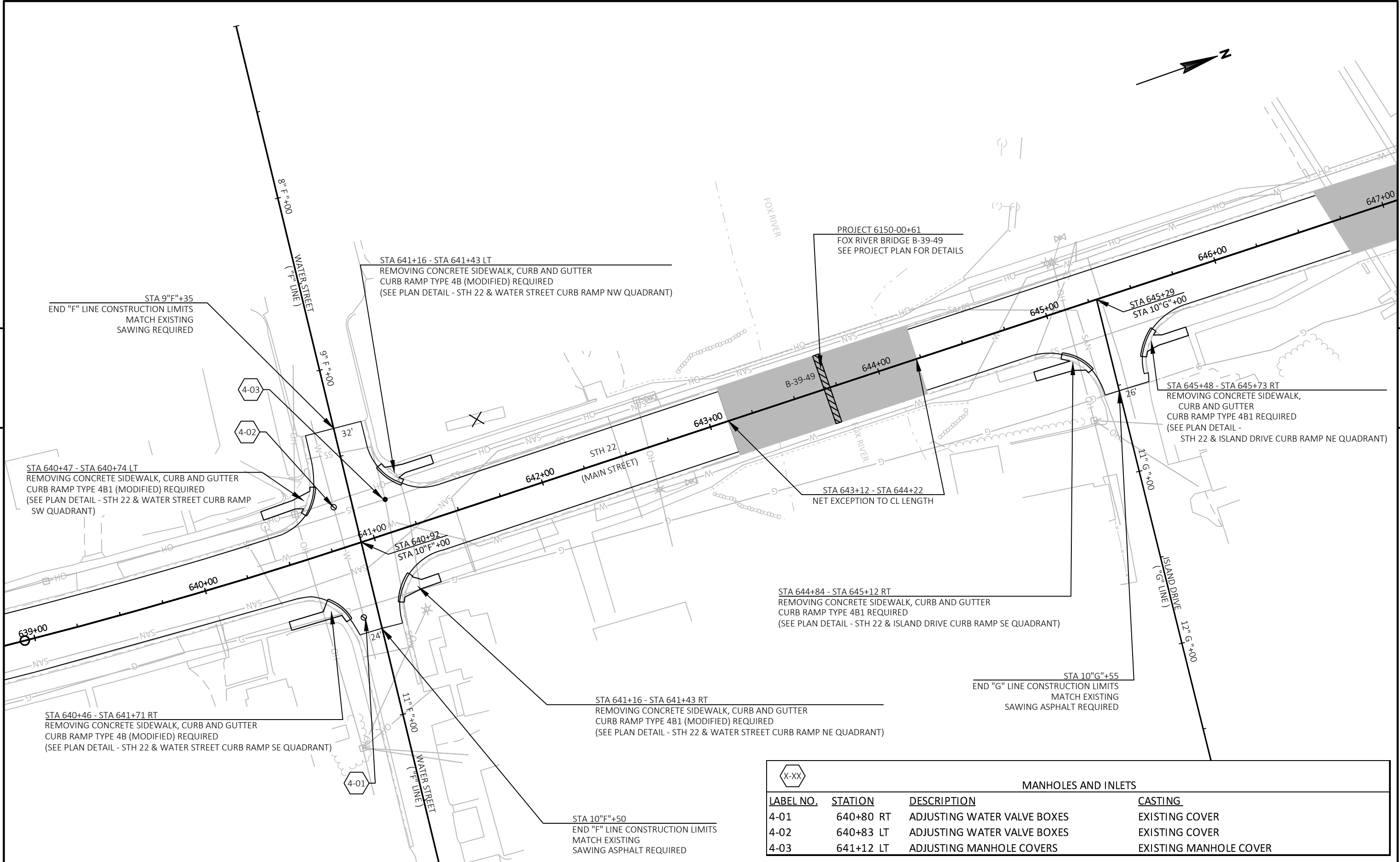


MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
3-01	632+59 LT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER
3-02	634+45 RT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER
3-03	636+76 LT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER



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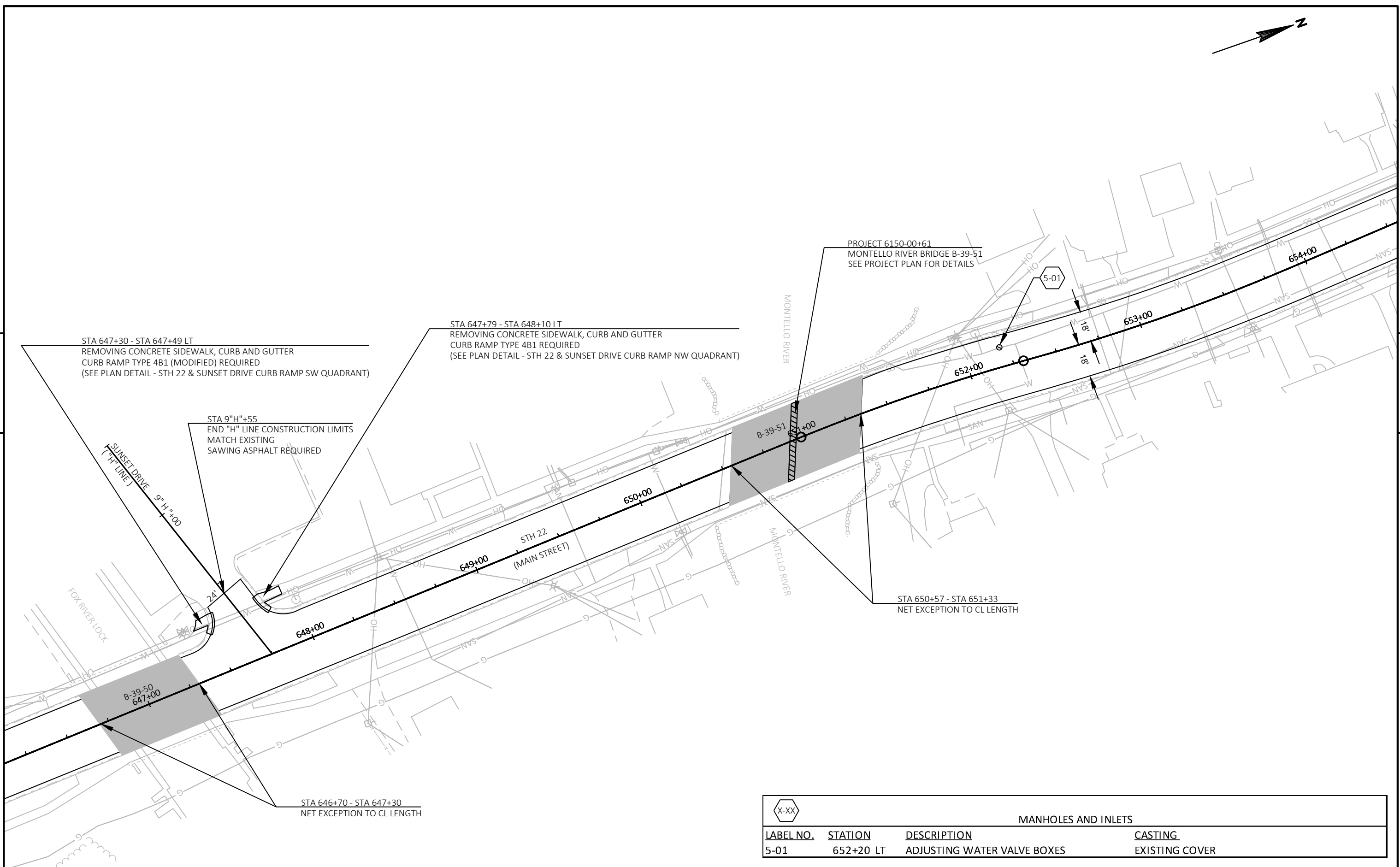


MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
4-01	640+80 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
4-02	640+83 LT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
4-03	641+12 LT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER



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MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
5-01	652+20 LT	ADJUSTING WATER VALVE BOXES	EXISTING COVER

PROJECT NO: 6054-05-75 HWY: STH 22 COUNTY: MARQUETTE PLAN SHEET E




5

5



END ASPHALTIC MILLING AND PAVING
 STA 662+05
 MATCH EXISTING
 SAWING ASPHALT REQUIRED

 MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
6-01	655+56 LT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
6-02	658+20 LT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
6-03	658+28 LT	ADJUSTING WATER VALVE BOXES	EXISTING COVER



STA 9"K"+45
REMOVING PEDESTRIAN PUSH BUTTON

STA 8"K"+85
END "K" LINE CONSTRUCTION LIMITS
MATCH EXISTING
SAWING CONCRETE REQUIRED

STA 9"K"+43 - STA 9"K"+62 RT
STA 668+38 - STA 668+54 LT
REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
CURB RAMP TYPE 3 (MODIFIED) REQUIRED
(SEE PLAN DETAIL - STH 22 & STH 23 CURB RAMPS SW QUADRANT)

EQUATION STA 8"K"+85 - STA 9"K"+23 BK=
STA 15+55.21 - STA 15+92 AHD
SEE STRUCTURE PLAN B-39-970

STA 9"K"+46 - STA 9"K"+60 LT
REMOVING CONCRETE SIDEWALK, CURB
AND GUTTER
CURB RAMP TYPE 7B (MODIFIED) REQUIRED
(SEE PLAN DETAIL - STH 22 & STH 23
NW CURB RAMP)

STA 668+43 LT
PEDESTRIAN PUSH BUTTON REQUIRED

STA 666+25 - STA 666+62 LT
REMOVING PAVEMENT
CONCRETE PAVEMENT 8-INCH REQUIRED
(SEE PLAN DETAIL - STAGED CONSTRUCTION SHEETS)

STA 668+38 MEDIAN
REMOVING PAVEMENT
CONCRETE MEDIAN SLOPED NOSE TYPE 2 REQUIRED
(SEE MEDIAN CONSTRUCTION DETAIL)

STA 665+79 - STA 666+13 LT
REMOVING PAVEMENT
CONCRETE PAVEMENT 8-INCH REQUIRED
(SEE PLAN DETAIL - STAGED CONSTRUCTION SHEETS)

STA 664+40 - STA 664+55 LT
REMOVING PAVEMENT
CONCRETE PAVEMENT 8-INCH REQUIRED
(SEE PLAN DETAIL - STAGED CONSTRUCTION SHEETS)

STA 669+45 LT
REMOVING SIDEWALK,
CURB AND GUTTER

CURB RAMP
TYPE 7B (MODIFIED)
REQUIRED
(SEE PLAN DETAIL -
STH 22 & STH 23
CURB RAMP
NE QUADRANT)

END ASPHALTIC MILLING AND PAVING
STA 662+05
MATCH EXISTING
SAWING ASPHALT REQUIRED

STA 666+94 - STA 670+34 RT & LT
STA 9"K"+42 - STA 10"K"+00 RT & LT
REMOVING PAVEMENT
CONCRETE PAVEMENT 8-INCH REQUIRED
(SEE PLAN DETAIL - STAGED CONSTRUCTION SHEETS)

STA 667+20 MEDIAN
REMOVING PAVEMENT
CONCRETE CORRUGATED MEDIAN REQUIRED
(SEE MEDIAN CONSTRUCTION DETAIL)

STA 662+75 - STA 662+81 RT
REMOVING PAVEMENT
CONCRETE PAVEMENT 8-INCH REQUIRED
(SEE PLAN DETAIL - STAGED CONSTRUCTION SHEETS)

STA 669+33 - STA 669-93 RT
REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
CURB RAMP TYPE 6 (MODIFIED) REQUIRED
(SEE PLAN DETAIL - STH 22 & STH 23 ISLAND/SE CURB RAMPS)

BEGIN ASPHALTIC MILLING AND PAVING
STA 670+97
MATCH EXISTING
SAWING ASPHALT REQUIRED



MANHOLES, INLETS, AND PULL BOX

LABEL NO.	STATION	DESCRIPTION	CASTING
7-01	666+36 LT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
7-02	668+30 LT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
7-03	668+46 LT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
7-04	669+51 RT	ADJUSTING PULL BOXES	EXISTING COVER
7-05	670+15 LT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER
7-06	670+20 LT	RECONSTRUCTING INLETS	INSIDE REPAIR ONLY; DO NOT RESET INLET COVER

PROJECT NO: 6054-05-75

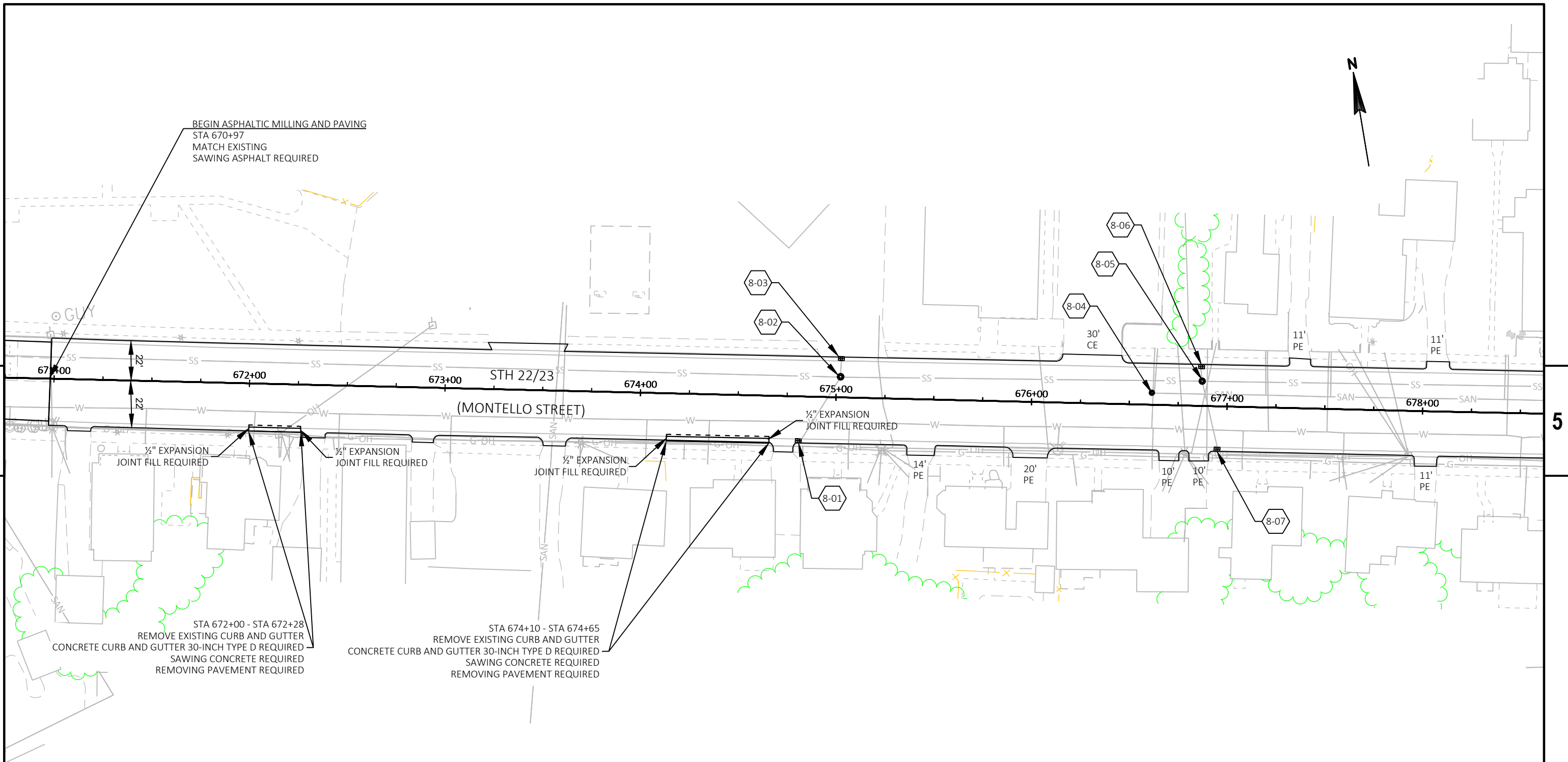
HWY: STH 22

COUNTY: MARQUETTE

PLAN

SHEET

E



BEGIN ASPHALTIC MILLING AND PAVING
 STA 670+97
 MATCH EXISTING
 SAWING ASPHALT REQUIRED

1/2" EXPANSION
 JOINT FILL REQUIRED

1/2" EXPANSION
 JOINT FILL REQUIRED

1/2" EXPANSION
 JOINT FILL REQUIRED

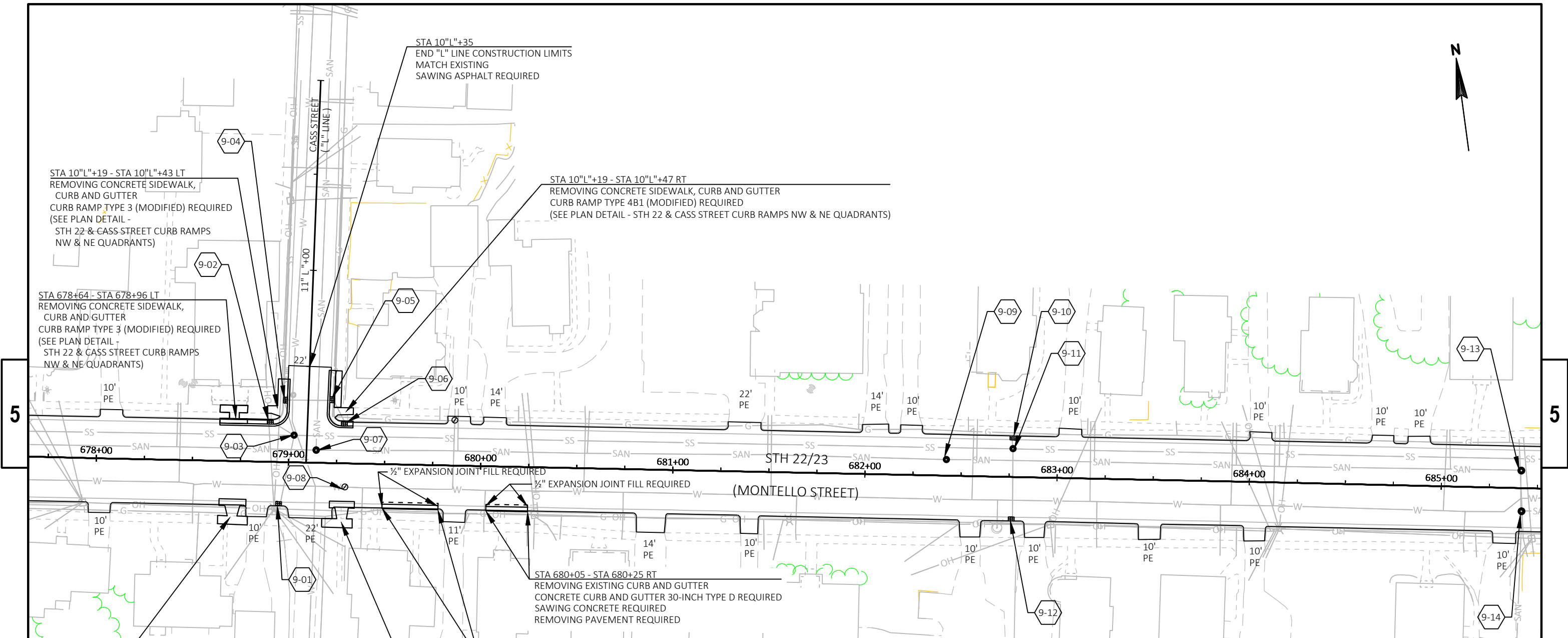
1/2" EXPANSION
 JOINT FILL REQUIRED

STA 672+00 - STA 672+28
 REMOVE EXISTING CURB AND GUTTER
 CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED
 SAWING CONCRETE REQUIRED
 REMOVING PAVEMENT REQUIRED

STA 674+10 - STA 674+65
 REMOVE EXISTING CURB AND GUTTER
 CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED
 SAWING CONCRETE REQUIRED
 REMOVING PAVEMENT REQUIRED

NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING CONCRETE PAVEMENT TO ALLOW FOR CURB FORMING. ADD BASE
 AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
8-01	674+75 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
8-02	675+00 LT	RECONSTRUCTING MANHOLES	MANHOLE COVER TYPE J REQUIRED
8-03	675+00 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
8-04	676+60 LT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
8-05	676+80 LT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER
8-06	676+80 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
8-07	676+85 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED

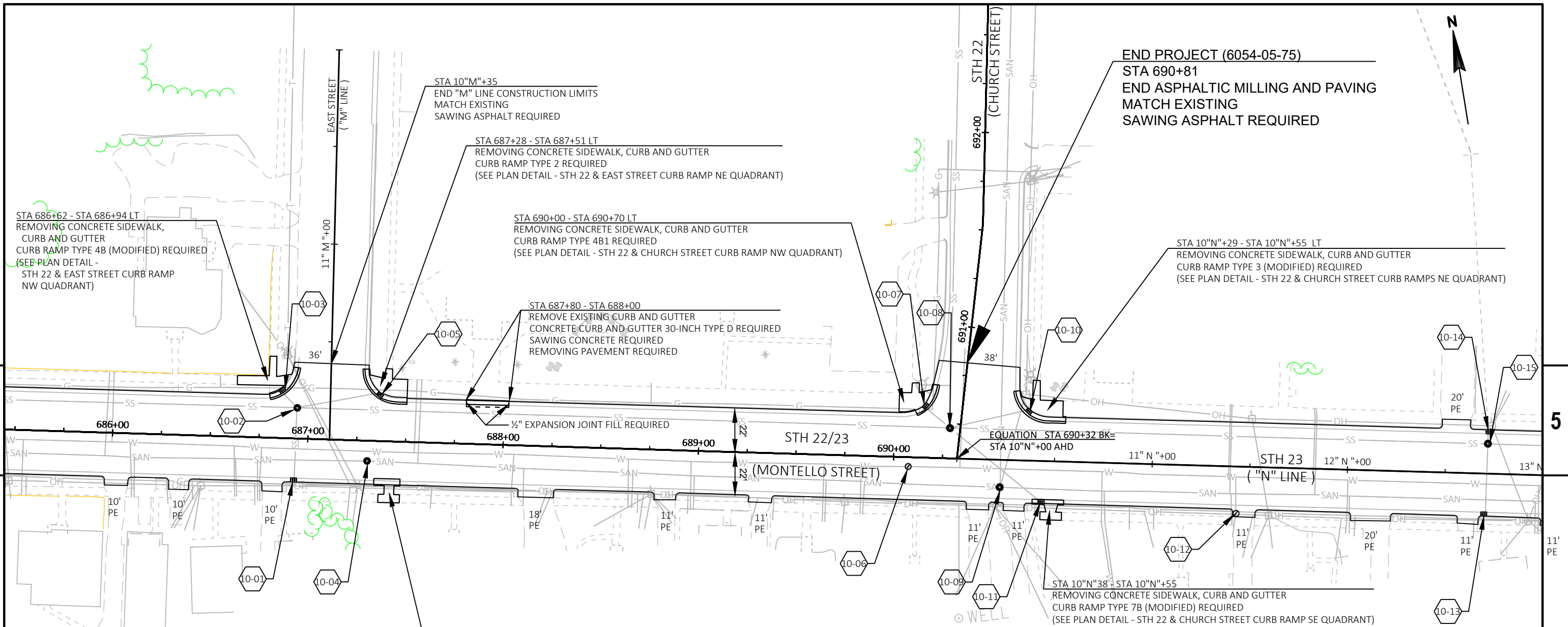


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NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING CONCRETE PAVEMENT TO ALLOW FOR CURB FORMING.
 ADD BASE AGG DENSE 3/4" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
9-01	678+90 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
9-02	678+90 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
9-03	679+03 LT	RECONSTRUCTING MANHOLES	MANHOLE COVER TYPE J REQUIRED
9-04	10"L"+32 LT	ADJUSTING INLET COVERS	EXISTING COVER
9-05	10"L"+32 RT	ADJUSTING INLET COVERS	EXISTING COVER
9-06	679+25 LT	ADJUSTING INLET COVERS	EXISTING COVER
9-07	679+10 LT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
9-08	679+25 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
9-09	682+40 LT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
9-10	682+75 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
9-11	682+75 LT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER
9-12	682+75 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
9-13	685+40 LT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
9-14	685+40 RT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER



END PROJECT (6054-05-75)
 STA 690+81
 END ASPHALTIC MILLING AND PAVING
 MATCH EXISTING
 SAWING ASPHALT REQUIRED

STA 686+62 - STA 686+94 LT
 REMOVING CONCRETE SIDEWALK,
 CURB AND GUTTER
 CURB RAMP TYPE 4B (MODIFIED) REQUIRED
 (SEE PLAN DETAIL -
 STH 22 & EAST STREET CURB RAMP
 NW QUADRANT)

STA 10"M"+35
 END "M" LINE CONSTRUCTION LIMITS
 MATCH EXISTING
 SAWING ASPHALT REQUIRED

STA 687+28 - STA 687+51 LT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 2 REQUIRED
 (SEE PLAN DETAIL - STH 22 & EAST STREET CURB RAMP NE QUADRANT)

STA 690+00 - STA 690+70 LT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 4B1 REQUIRED
 (SEE PLAN DETAIL - STH 22 & CHURCH STREET CURB RAMP NW QUADRANT)

STA 687+80 - STA 688+00
 REMOVE EXISTING CURB AND GUTTER
 CONCRETE CURB AND GUTTER 30-INCH TYPE D REQUIRED
 SAWING CONCRETE REQUIRED
 REMOVING PAVEMENT REQUIRED

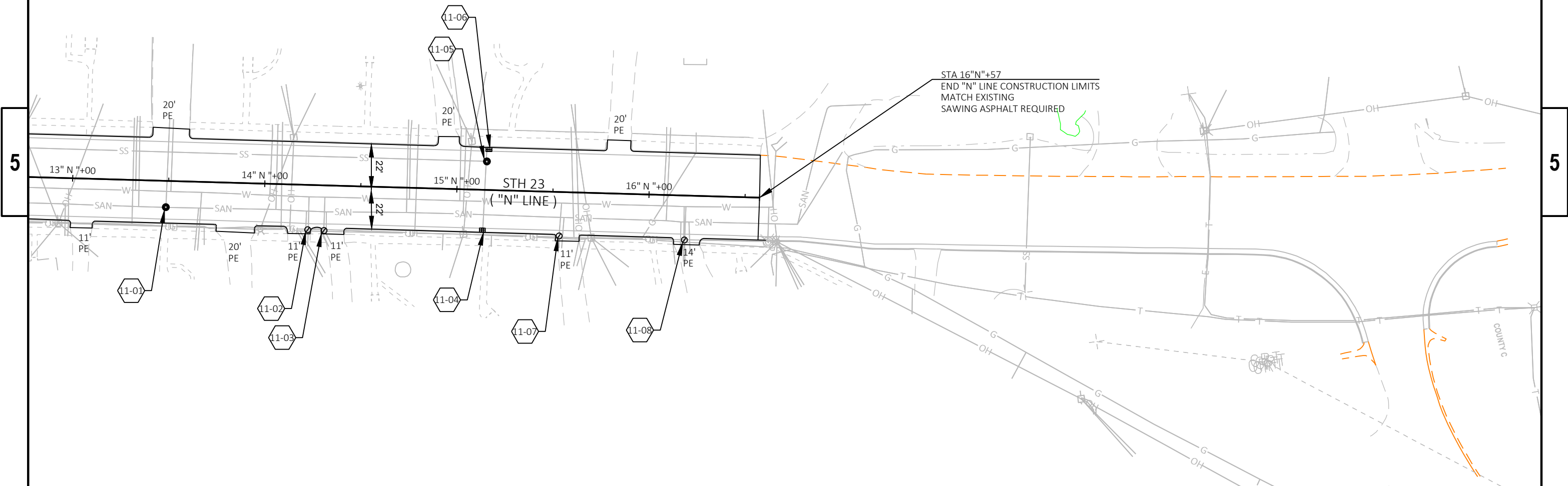
STA 10"N"+29 - STA 10"N"+55 LT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 3 (MODIFIED) REQUIRED
 (SEE PLAN DETAIL - STH 22 & CHURCH STREET CURB RAMPS NE QUADRANT)


STA 687+35 - STA 687+48 RT
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 7B REQUIRED
 (SEE PLAN DETAIL - STH 22 & EAST STREET CURB RAMP SW QUADRANT)

STA 10"N"38 - STA 10"N"+55
 REMOVING CONCRETE SIDEWALK, CURB AND GUTTER
 CURB RAMP TYPE 7B (MODIFIED) REQUIRED
 (SEE PLAN DETAIL - STH 22 & CHURCH STREET CURB RAMP SE QUADRANT)

NOTE:
 REPLACE CONCRETE CURB AND GUTTER AT EXISTING ELEVATION
 REMOVE 1-FOOT WIDTH OF EXISTING CONCRETE PAVEMENT TO ALLOW FOR CURB FORMING.
 ADD BASE AGG DENSE ¾" AS NEEDED AND PATCH WITH ASPHALT BINDER LAYER PRIOR TO PLACING OVERLAY.

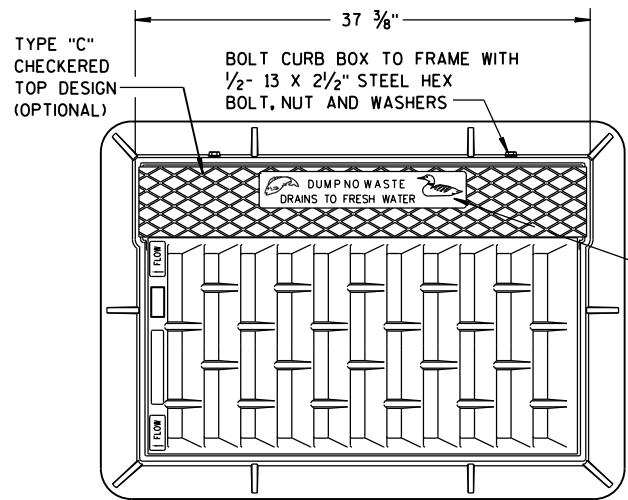
MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
10-01	686+90 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-02	686+90 LT	RECONSTRUCTING MANHOLES	MANHOLE COVER TYPE J REQUIRED
10-03	686+81 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-04	687+30 RT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
10-05	687+38 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-06	690+10 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
10-07	690+60 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-08	690+47 LT	RECONSTRUCTING MANHOLES	MANHOLE COVER TYPE J REQUIRED
10-09	10"N"+22 RT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
10-10	10"N"+37 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-11	10"N"+44 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-12	11"N"+44 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
10-13	12"N"+69 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-14	12"N"+70 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
10-15	12"N"+70 LT	RECONSTRUCTING MANHOLES	MANHOLE COVER TYPE J REQUIRED



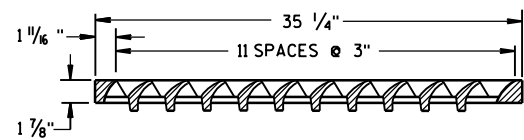
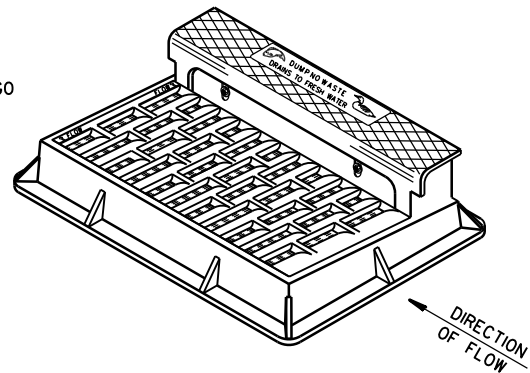
 MANHOLES AND INLETS			
LABEL NO.	STATION	DESCRIPTION	CASTING
11-01	13"N"+50 RT	ADJUSTING SANITARY MANHOLE COVERS	EXISTING MANHOLE COVER
11-02	14"N"+21 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
11-03	14"N"+32 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
11-04	15"N"+13 RT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
11-05	15"N"+14 LT	ADJUSTING MANHOLE COVERS	EXISTING MANHOLE COVER
11-06	15"N"+14 LT	RECONSTRUCTING INLETS	INLET COVER TYPE A REQUIRED
11-07	15"N"+54 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER
11-08	16"N"+20 RT	ADJUSTING WATER VALVE BOXES	EXISTING COVER

Standard Detail Drawing List

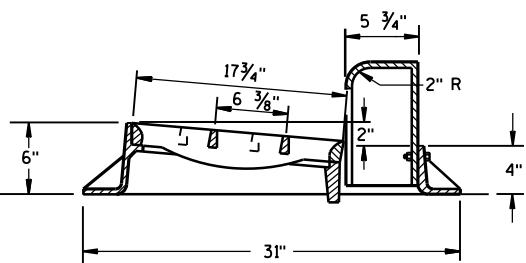
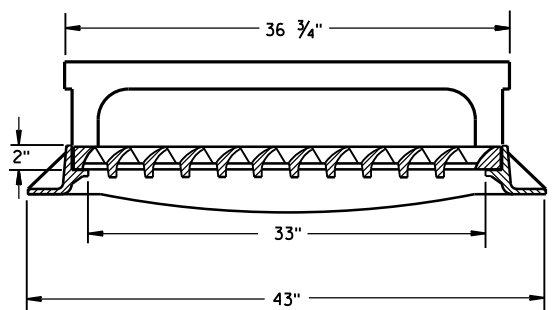
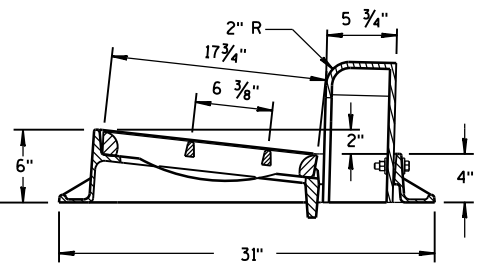
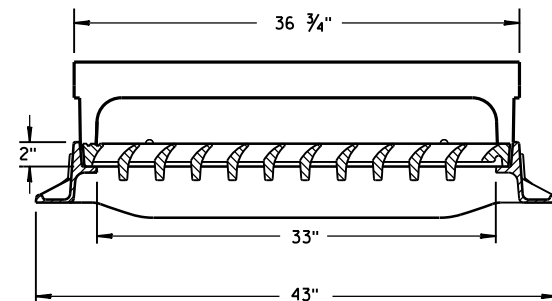
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08D01-20A	CONCRETE CURB & GUTTER
08D01-20B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-19A	CURB RAMPS TYPES 1 AND 1-A
08D05-19B	CURB RAMPS TYPES 2 AND 3
08D05-19C	CURB RAMPS TYPES 4A AND 4A1
08D05-19D	CURB RAMPS TYPE 4B AND 4B1
08D05-19E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-19G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09B04-11	PULL BOX
09C02-07	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09E07-05	TRAFFIC SIGNAL STANDARD PEDESTRIAN AND FLASHER TYPICAL MOUNTING DETAILS
09G02-04C	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
11B01-05	CONCRETE CORRUGATED MEDIAN
11B02-02	CONCRETE MEDIAN NOSE
13C01-18	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C09-14A	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-14B	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-14C	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C13-08	URBAN DOWELED CONCRETE PAVEMENT
13C14-06A	BASE PATCHING CONCRETE
13C14-06B	BASE PATCHING CONCRETE
13C14-06C	BASE PATCHING CONCRETE
14B07-15A	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15B	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15C	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15D	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15E	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15F	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15G	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15H	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B07-15I	CONCRETE BARRIER TEMPORARY PRECAST, 12' -6"
14B08-02A	CRASH CUSHION/SAND BARREL ARRAY AND OTHER TEMPORARY BARRIER LAYOUT DETAILS
14B08-02B	CRASH CUSHION/SAND BARREL ARRAY AND OTHER TEMPORARY BARRIER LAYOUT DETAILS
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-04	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C05-04	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C07-14B	PAVEMENT MARKING WORDS
15C07-14C	PAVEMENT MARKING ARROWS
15C08-18A	LONGITUDINAL MARKING (MAINLINE)
15C08-18B	PAVEMENT MARKING (TURN LANES)
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-06	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-05A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C27-02A	DOUBLE ARROW WARNING SIGN PLACEMENT
15C27-02B	PAVEMENT MARKING (ISLANDS)
15C33-03	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-02A	PAVEMENT MARKING (INTERSECTIONS)
15D28-03	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-03A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-03B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



**NOTE:
GRATE IS REVERSIBLE.**

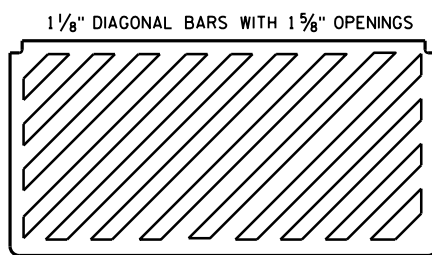


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"



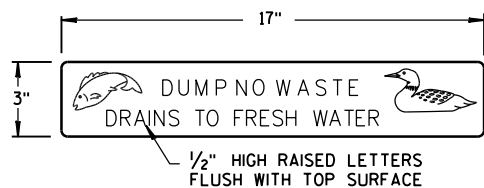
TYPE "H"

NOTE: EITHER CASTING IS ACCEPTABLE

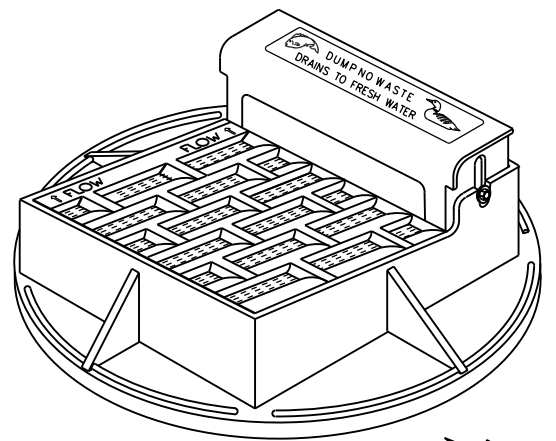


**SPECIAL GRATE FOR
TYPE "H" COVER**

(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

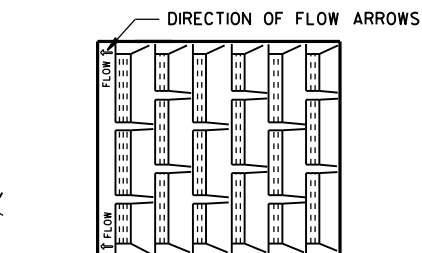
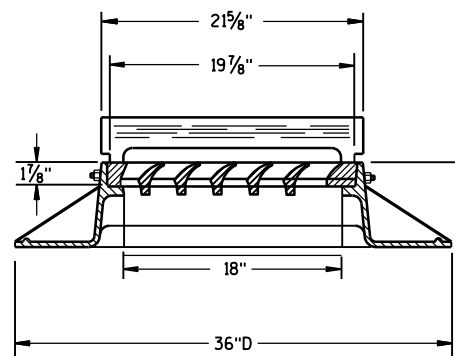


LOGO DETAIL

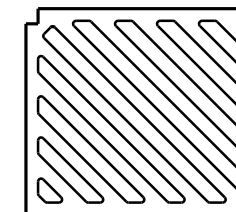


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

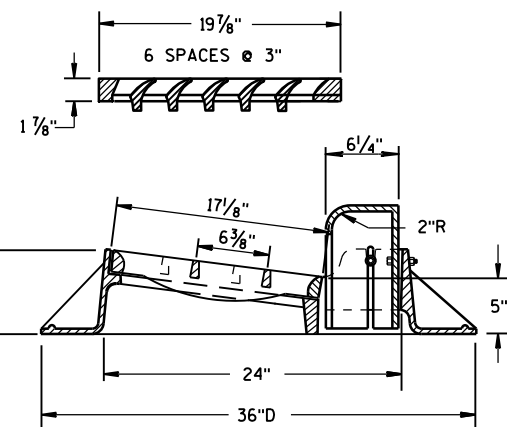
**NOTE:
GRATE IS REVERSIBLE.**



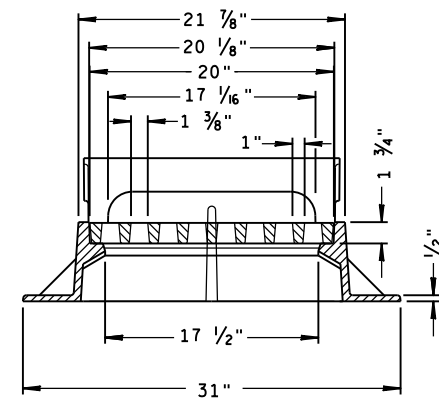
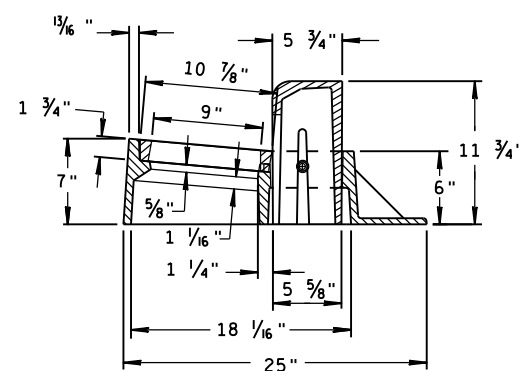
**1" DIAGONAL BARS
WITH 1 1/2" OPENINGS**



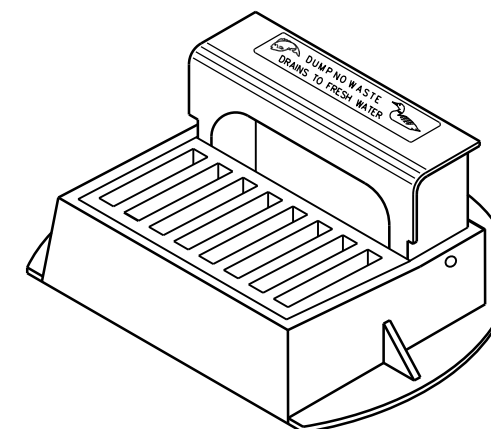
**SPECIAL GRATE FOR
TYPE "A" COVER**
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



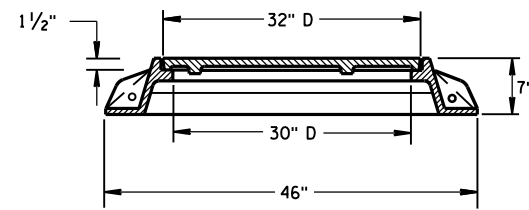
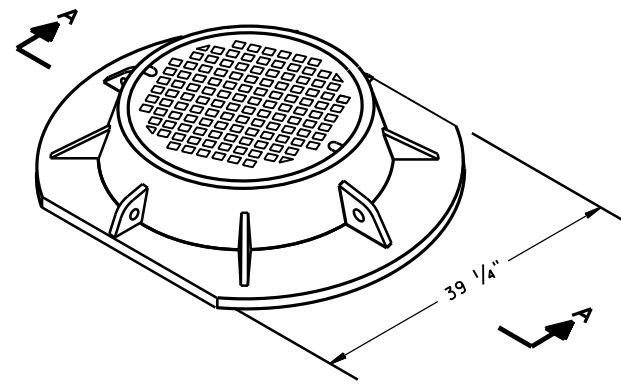
TYPE "Z"



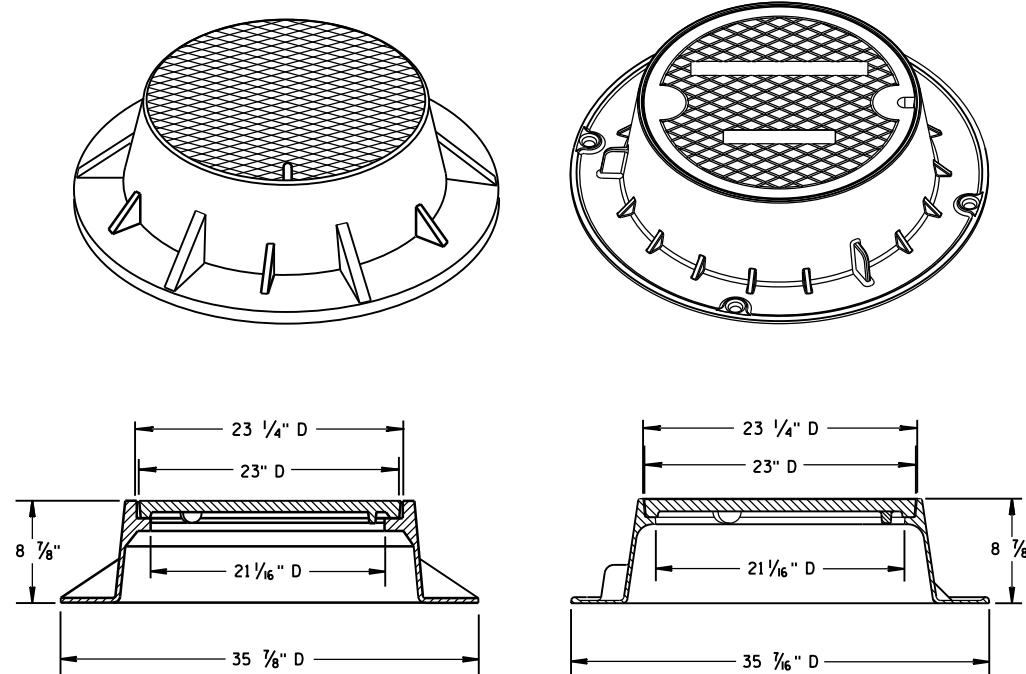
**INLET COVERS
TYPE A, H, A-S, H-S & Z**

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

APPROVED
11-27-13
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA

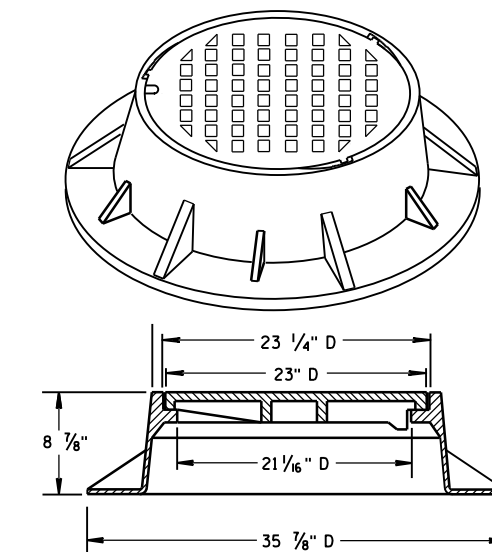
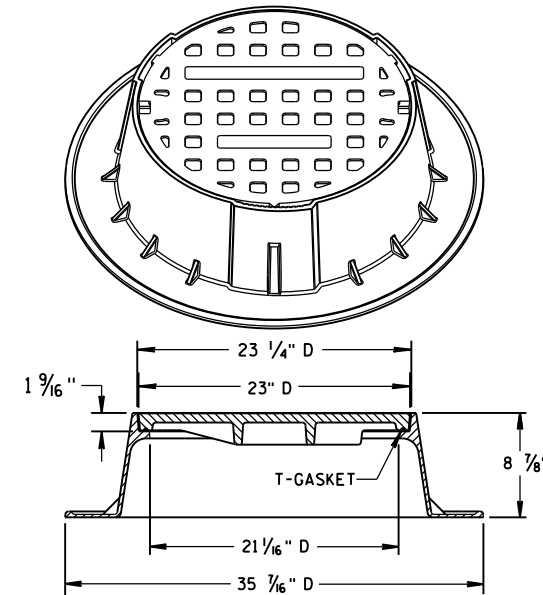


SECTION A-A
TYPE "K"



TYPE "J"

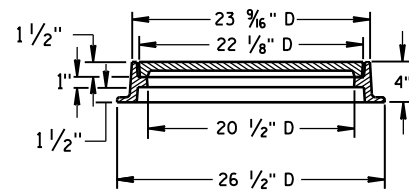
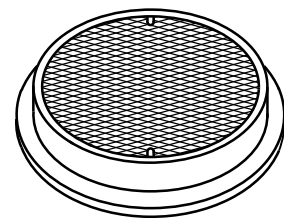
NOTE: EITHER CASTING IS ACCEPTABLE



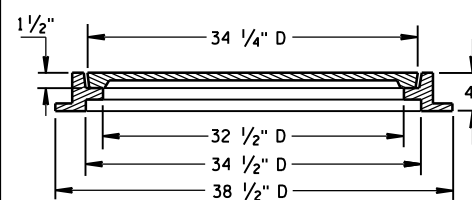
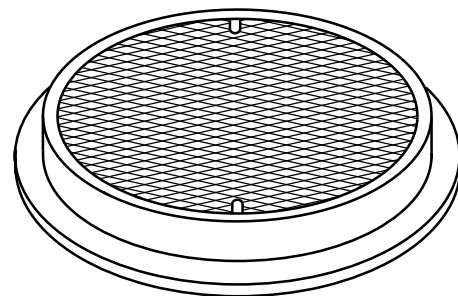
TYPE "J" SPECIAL

TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

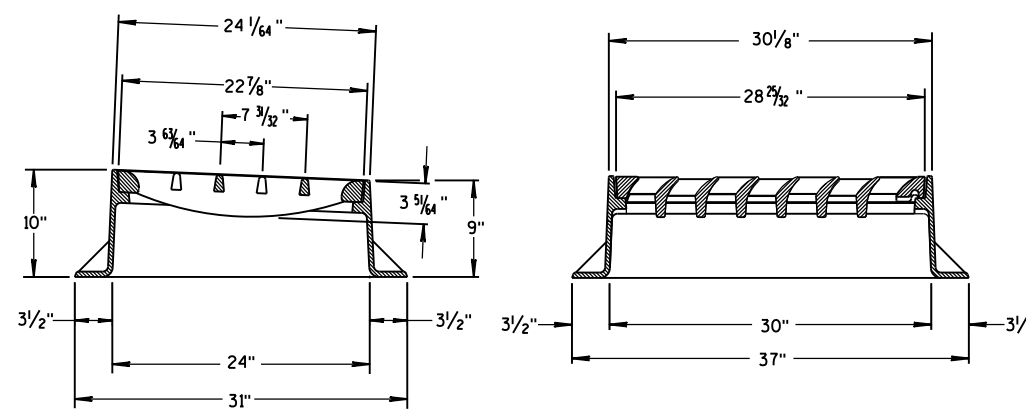
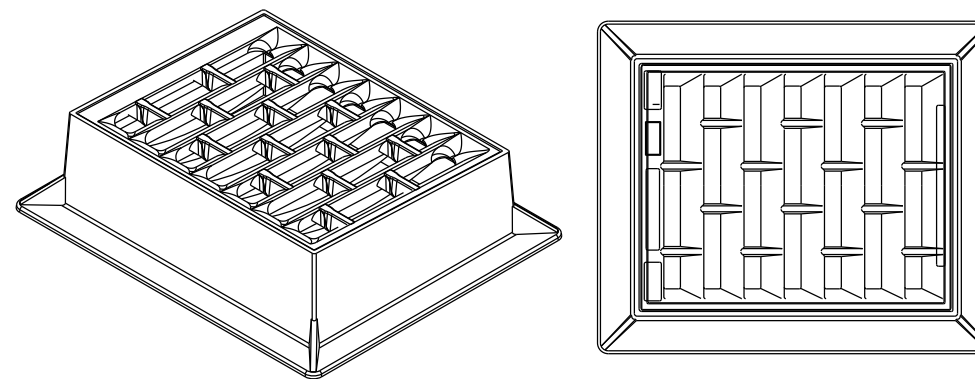
NOTE: EITHER CASTING IS ACCEPTABLE



TYPE "L"



TYPE "M"



INLET COVER TYPE "BW"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

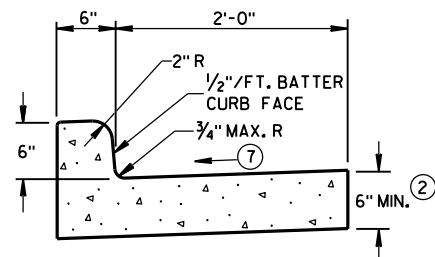
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

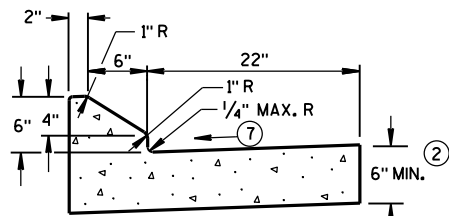
INLET COVER TYPE BW
MANHOLE COVERS, TYPE K,
J, J-S, L & M

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

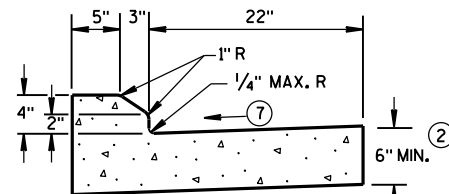
APPROVED
11/27/2013 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



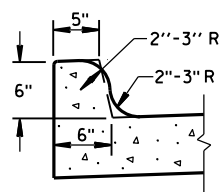
TYPES A^① & D



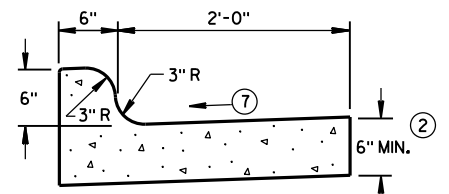
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

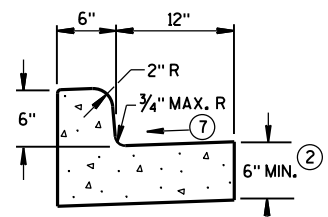


TYPES K^① & L
(OPTIONAL CURB SHAPE)



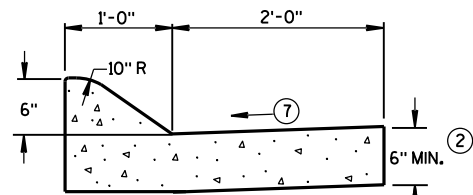
TYPES K^① & L

CONCRETE CURB & GUTTER 30"

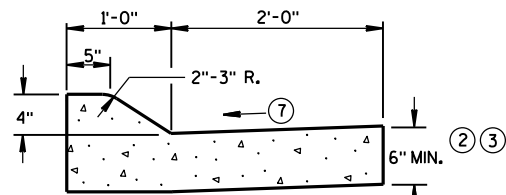


TYPES A^① & D

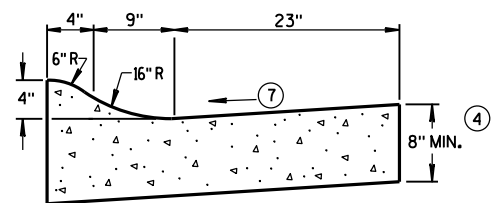
CONCRETE CURB & GUTTER 18"



6" SLOPED CURB TYPES A^① & D

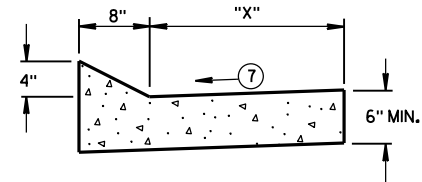


4" SLOPED CURB TYPES A^① & D



4" SLOPED CURB TYPES R^① & T^⑤

CONCRETE CURB & GUTTER 36"



TYPES TBT & TBTT^①

CONCRETE CURB & GUTTER

TBT & TBTT	"X"
30"	22"
36"	28"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

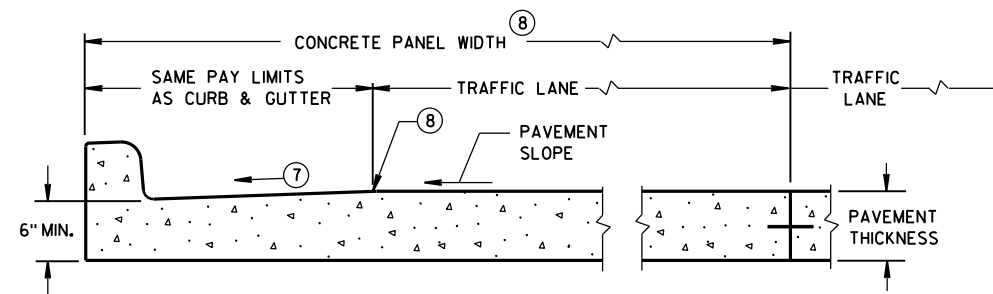
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

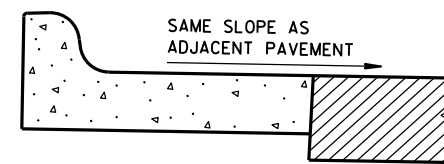
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'

* BIKE LANE IS NOT SHOWN.



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER

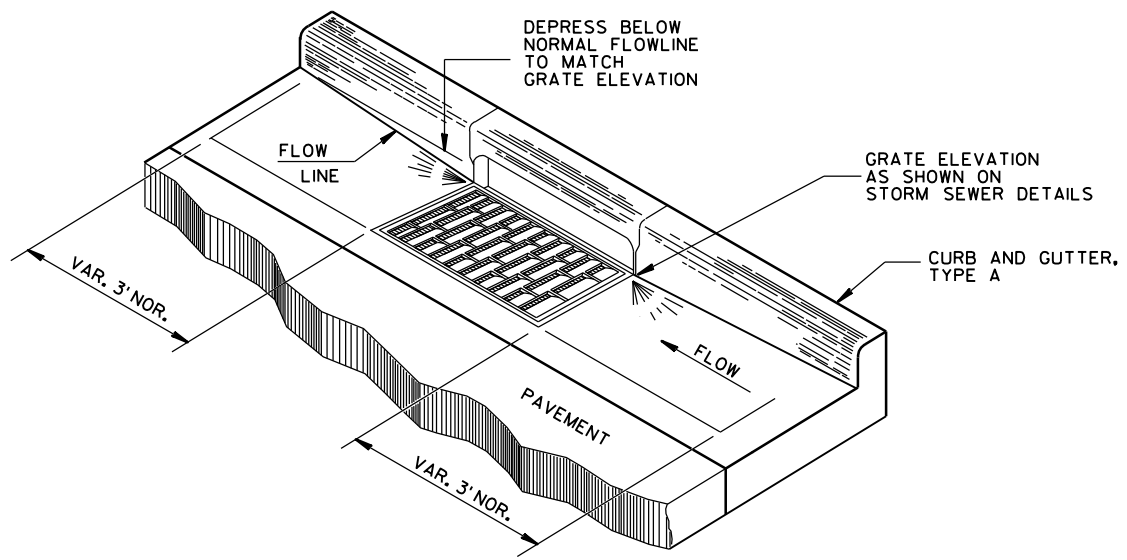


REVERSE SLOPE GUTTER^⑥

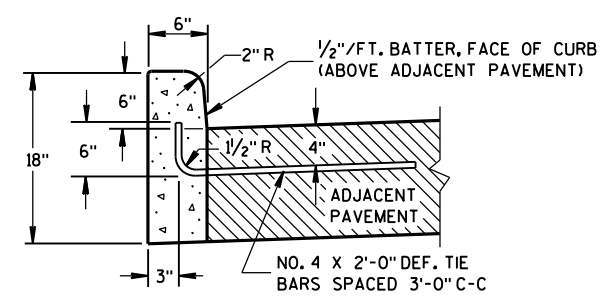
(TYPICAL FOR ALL CURB & GUTTER TYPES)

CONCRETE CURB & GUTTER

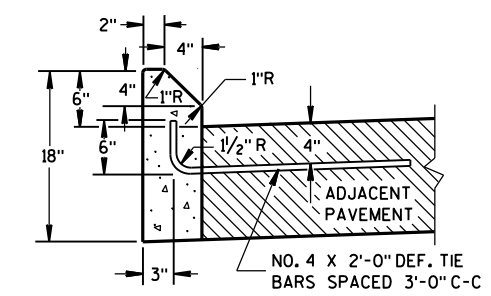
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



DETAIL OF CURB AND GUTTER AT INLETS
(TYPE H INLET COVER SHOWN)



TYPES A^① & D

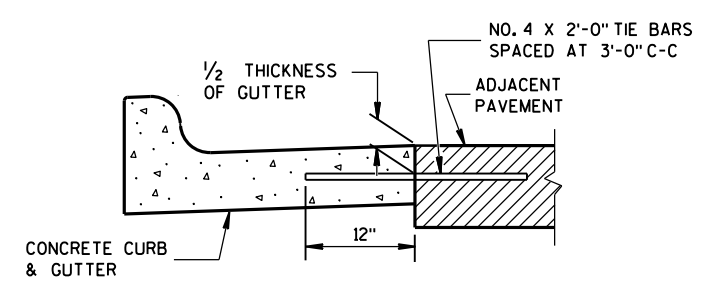


TYPES G^① & J

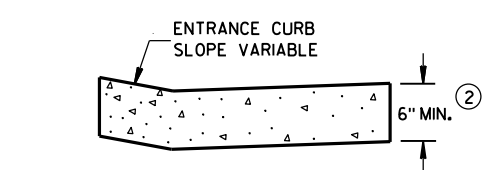
GENERAL NOTES

- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.
- PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.
- UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.
 - ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
 - ③ REFER TO SDD 8D18 AND SDD 8D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

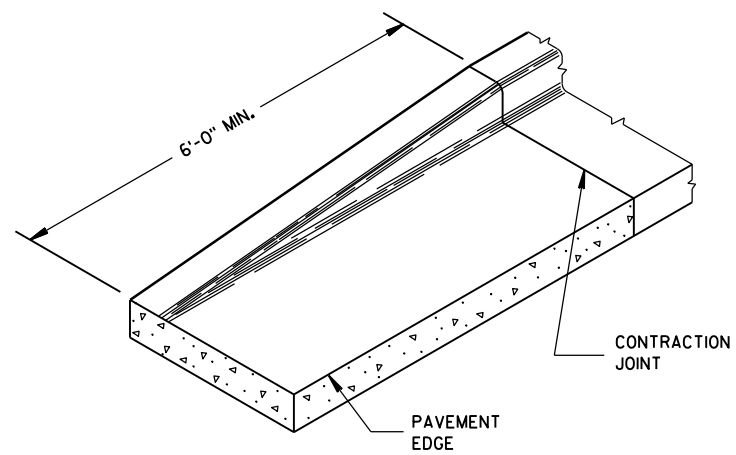
CONCRETE CURB



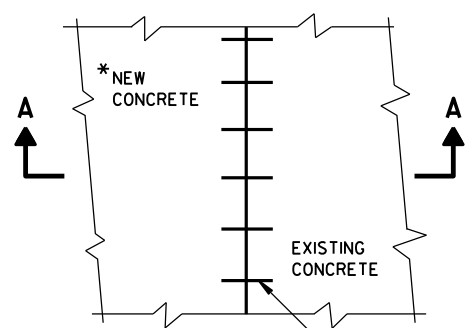
TYPICAL TIE BAR LOCATION^①



DRIVEWAY ENTRANCE CURB^⑨
(WHEN DIRECTED BY THE ENGINEER)



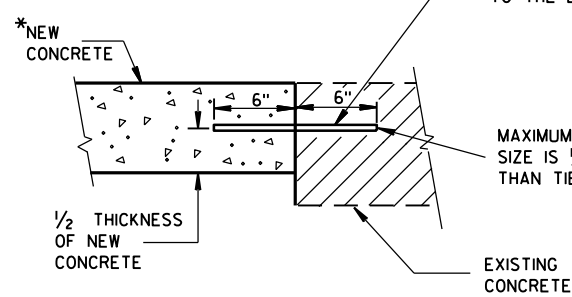
END SECTION CURB & GUTTER



PLAN VIEW

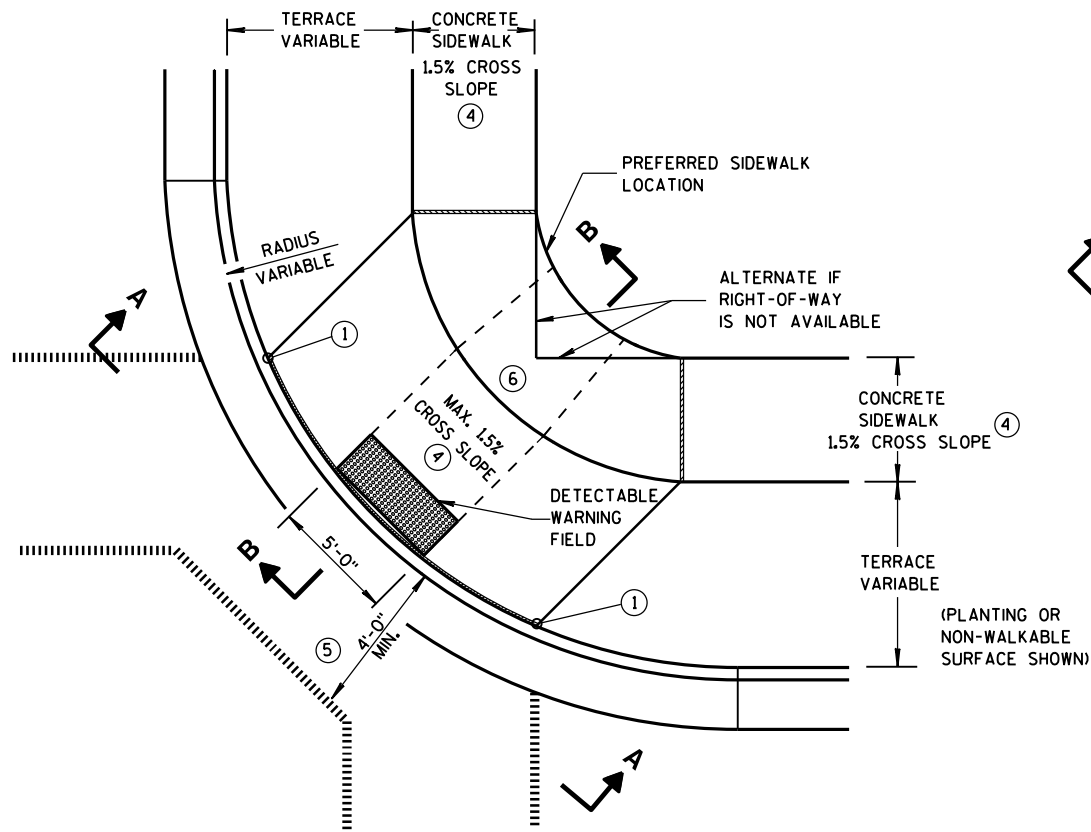
* NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

NO. 6 TIE BARS SPACED 2'-6" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT.

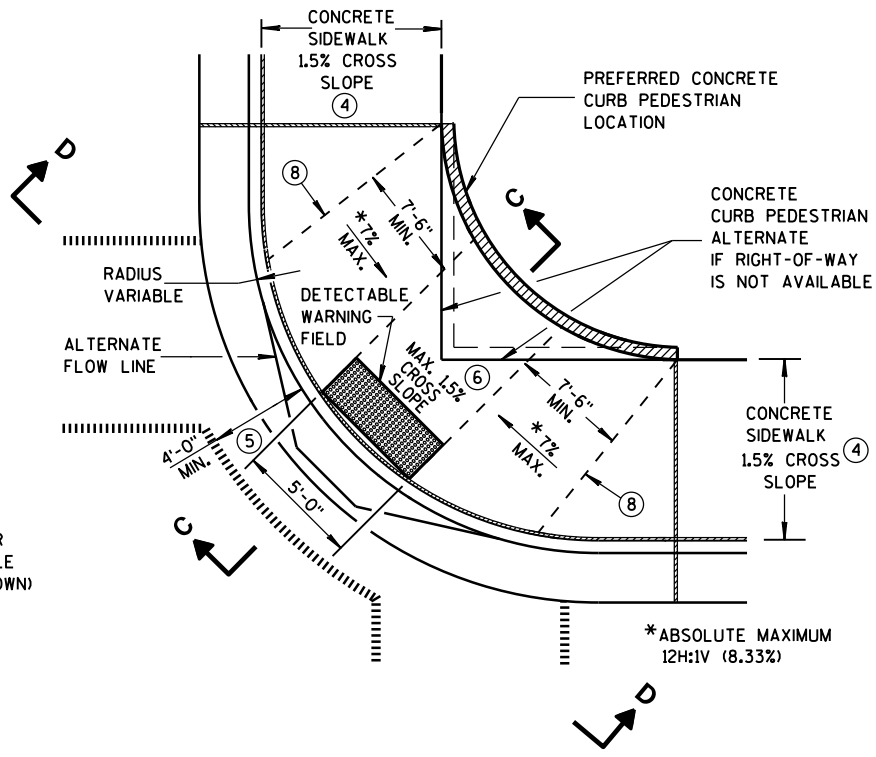


SECTION A-A
TIE BARS DRILLED INTO EXISTING PAVEMENT

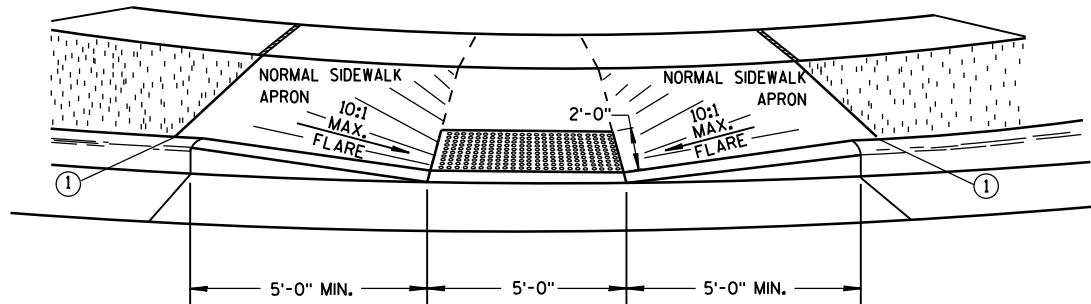
CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June, 2017 DATE	/s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	



**PLAN VIEW
TYPE 1 RAMP**
(CENTER OF CORNER RADIUS)

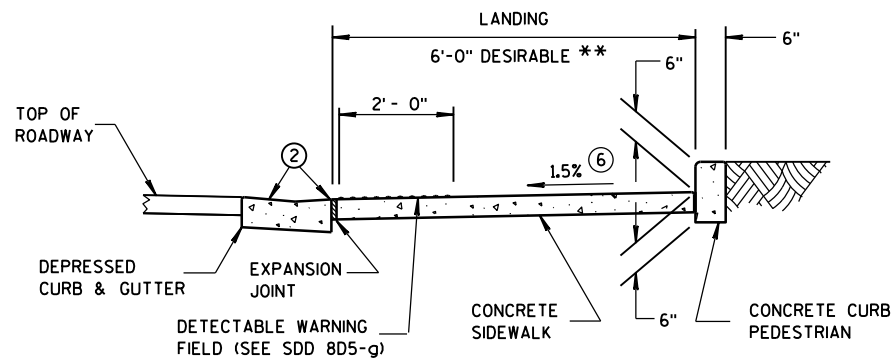


**PLAN VIEW
TYPE 1-A RAMP**
(NO TERRACE)

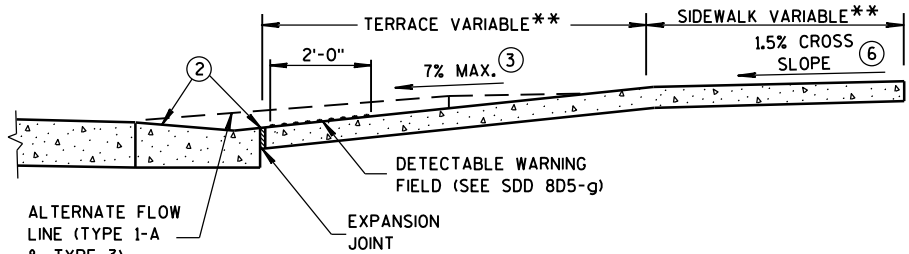


VIEW A-A

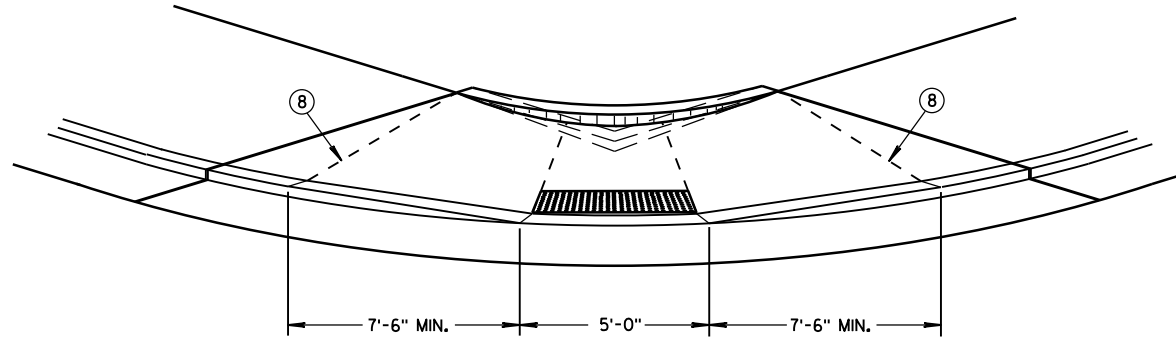
** WIDTH SHOWN ELSEWHERE
IN THE PLANS



SECTION C-C



SECTION B-B



VIEW D-D

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

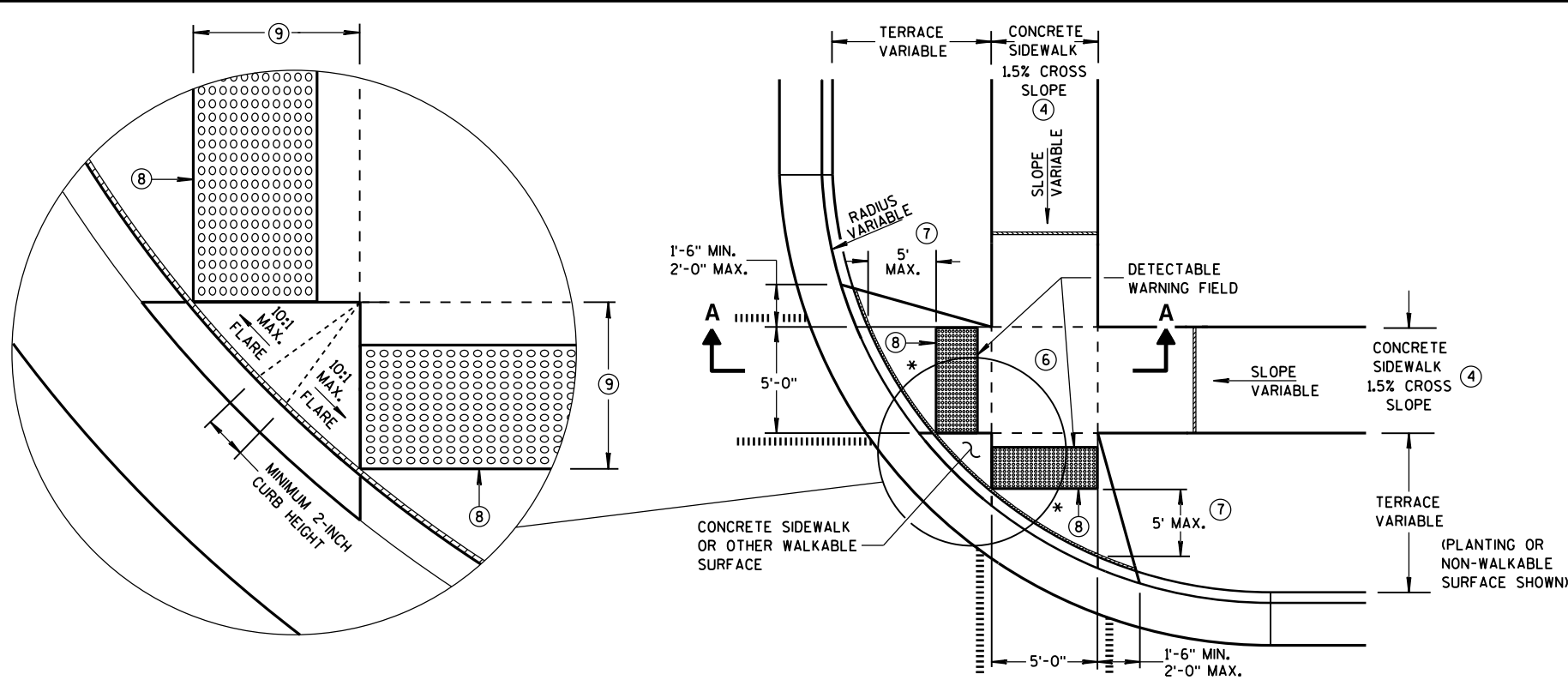
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA. (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT

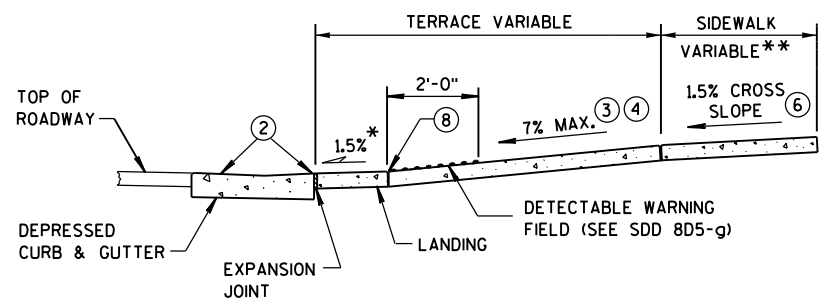
**CURB RAMPS
TYPES 1 AND 1-A**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



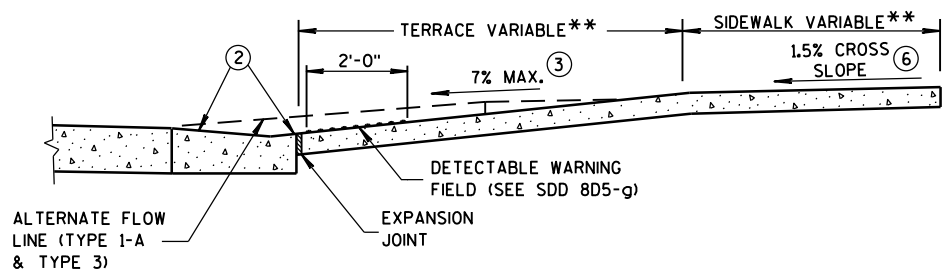
**PLAN VIEW
TYPE 2 RAMP**
(ON LINE WITH SIDEWALK)

* MAXIMUM 2.0% SLOPE
IN ALL DIRECTIONS IN
FRONT OF GRADE BREAK

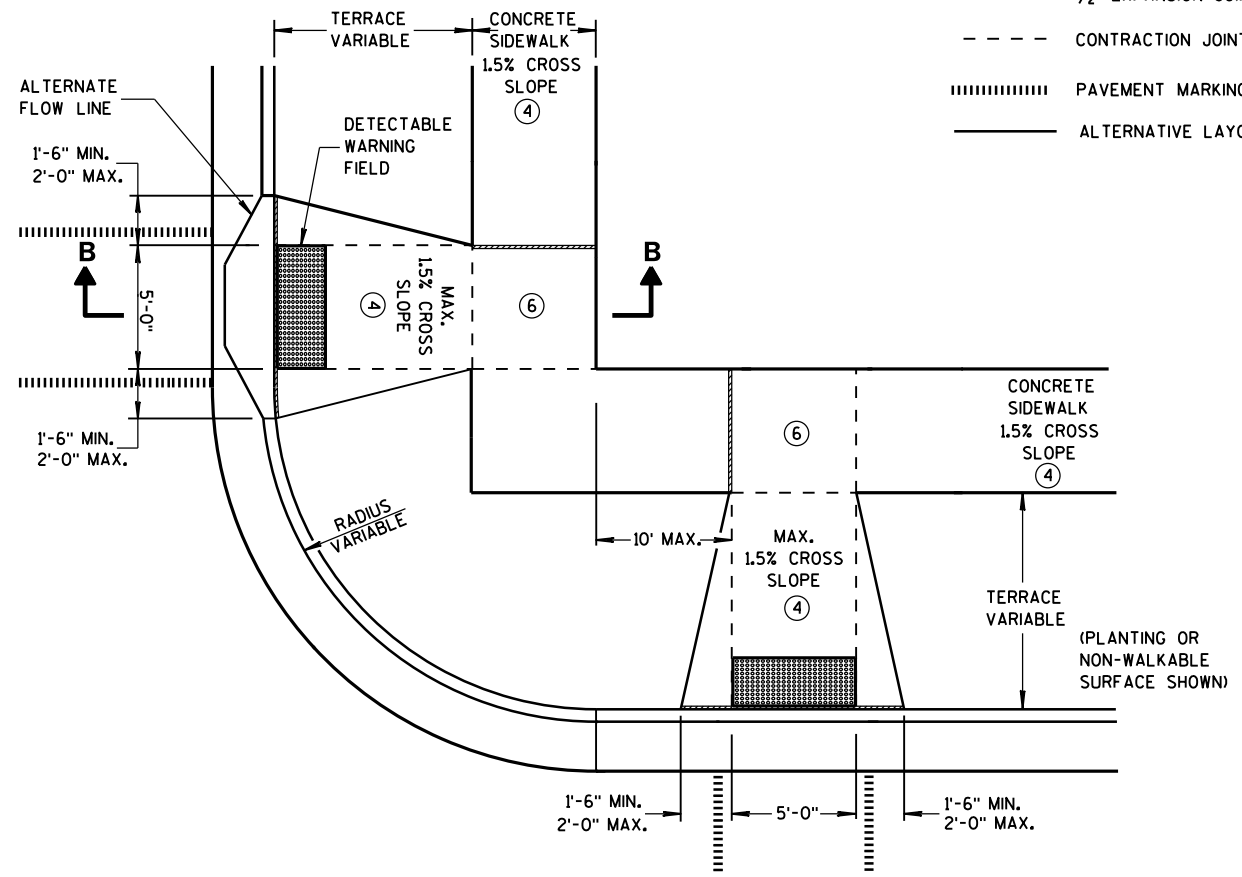


SECTION A-A

** WIDTH SHOWN ELSEWHERE
IN THE PLANS



SECTION B-B



**PLAN VIEW
TYPE 3 RAMP**
(OUTSIDE OF CROSSWALK AREA)

GENERAL NOTES

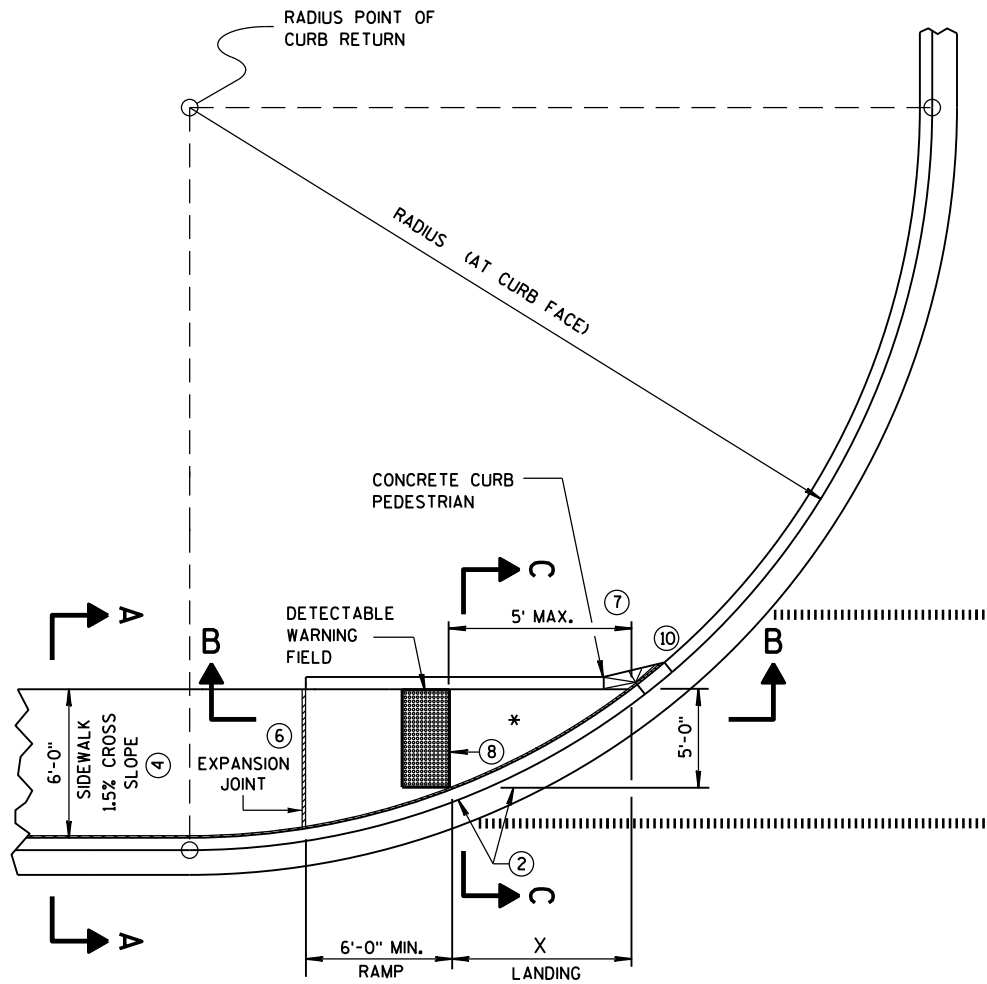
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN THIS DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT

**CURB RAMPS
TYPES 2 AND 3**

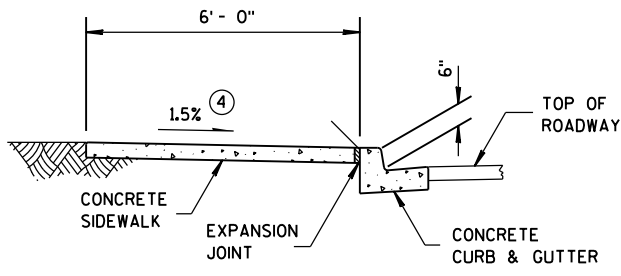
STATE OF WISCONSIN
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**CURB RAMP TYPE 4A
PLAN VIEW**

RADIUS (AT CURB FACE)	X
10 FEET	4'-7"
15 FEET	6'-5½"

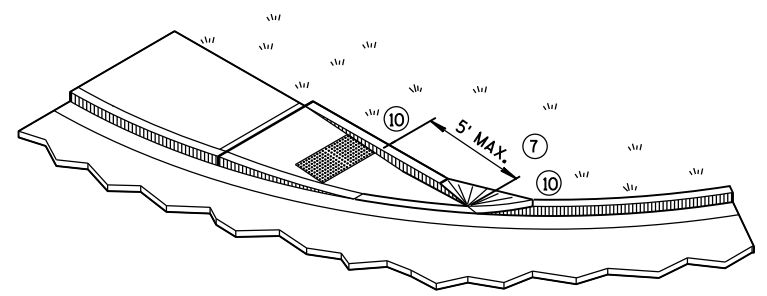
INTERMEDIATE RADII CAN BE INTERPOLATED



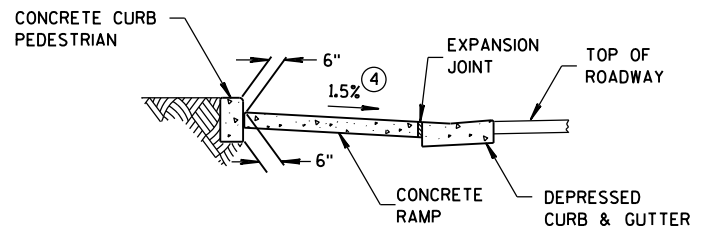
SECTION A-A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

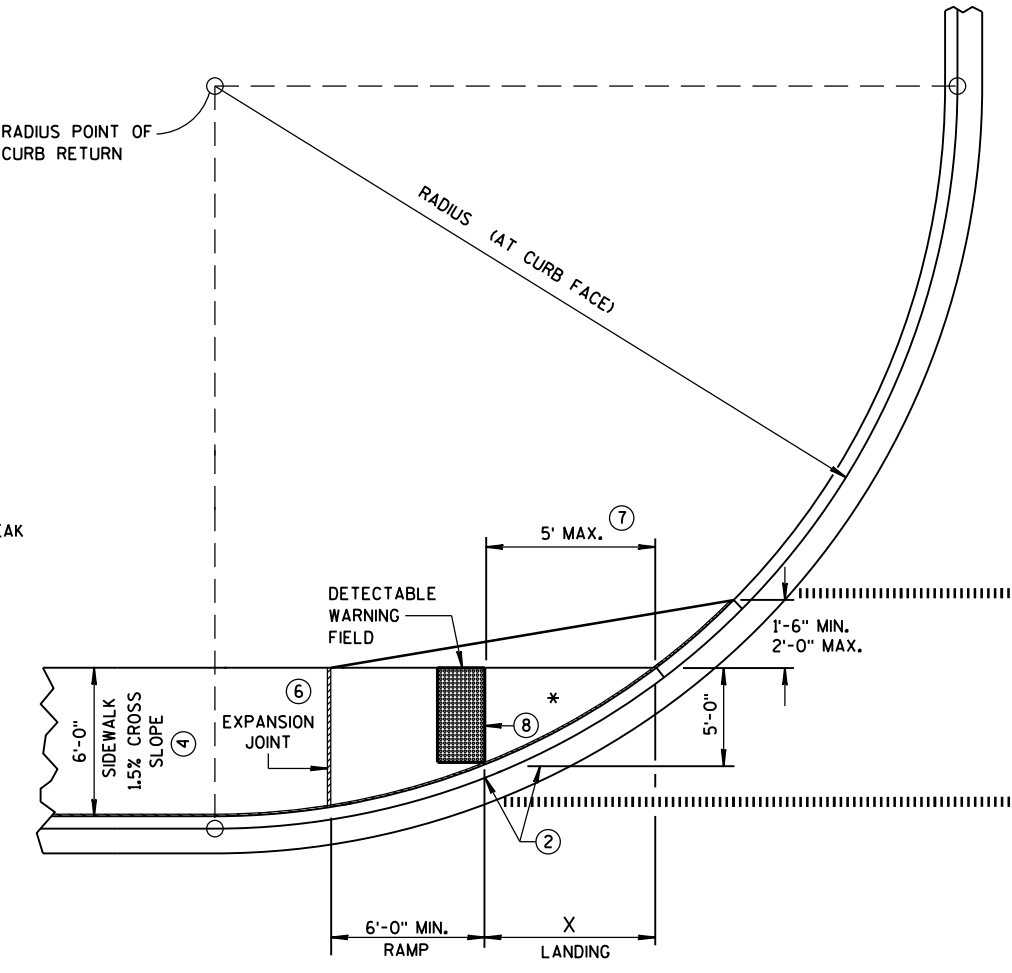


ISOMETRIC VIEW FOR TYPE 4A

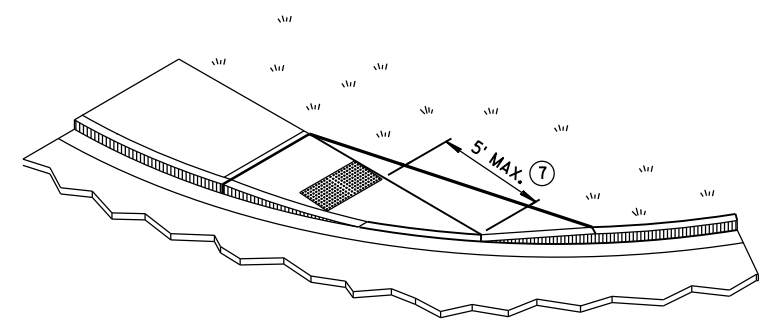


SECTION C-C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK



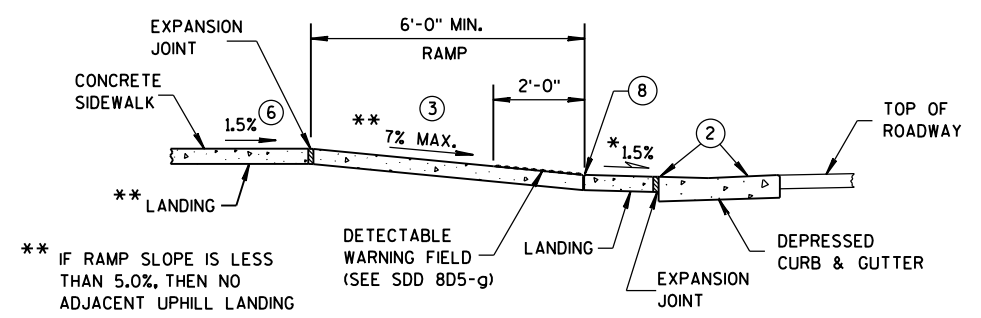
**CURB RAMP TYPE 4A1
PLAN VIEW**



ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- - - CONTRACTION JOINT FIELD LOCATED
- ▤ PAVEMENT MARKING CROSSWALK (WHITE)

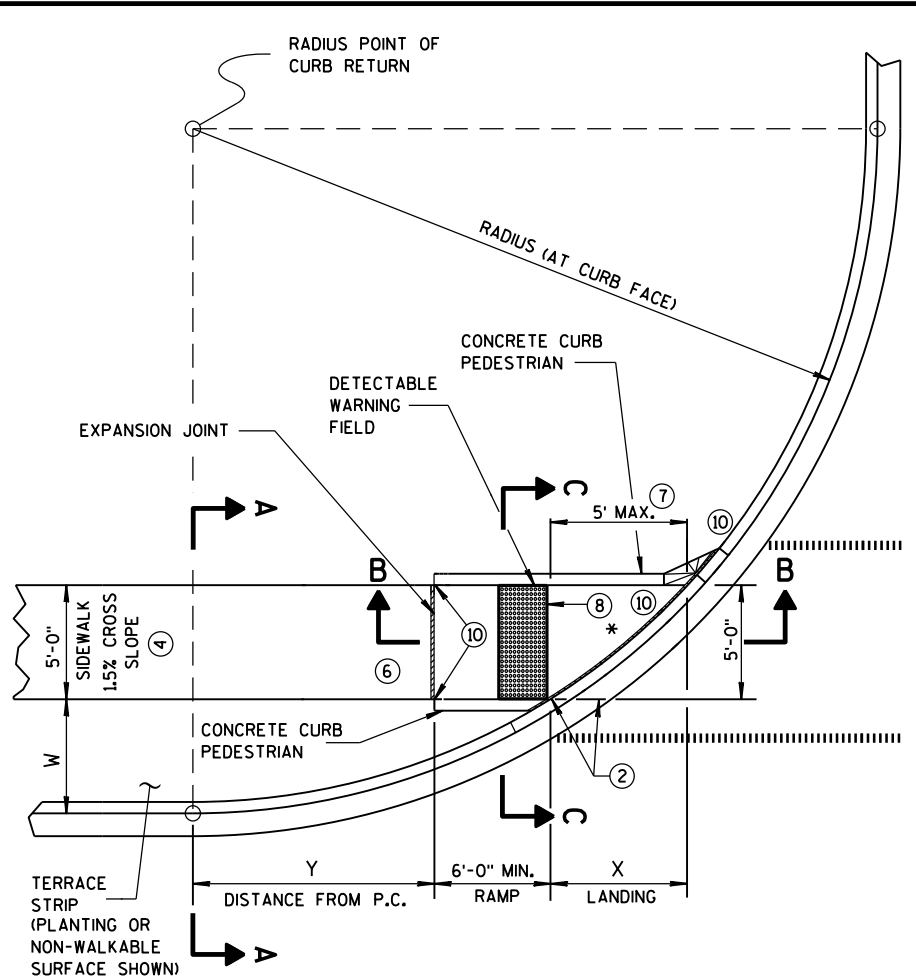


SECTION B-B FOR TYPE 4A

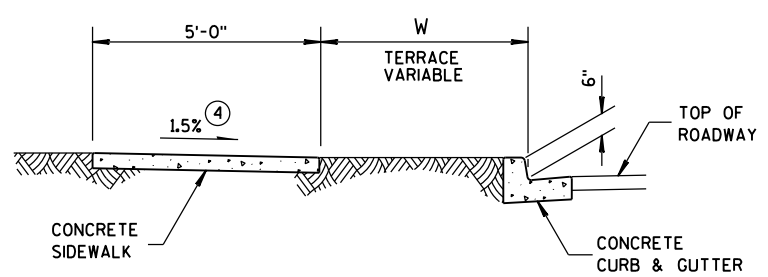
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

**CURB RAMPS
TYPES 4A AND 4A1**

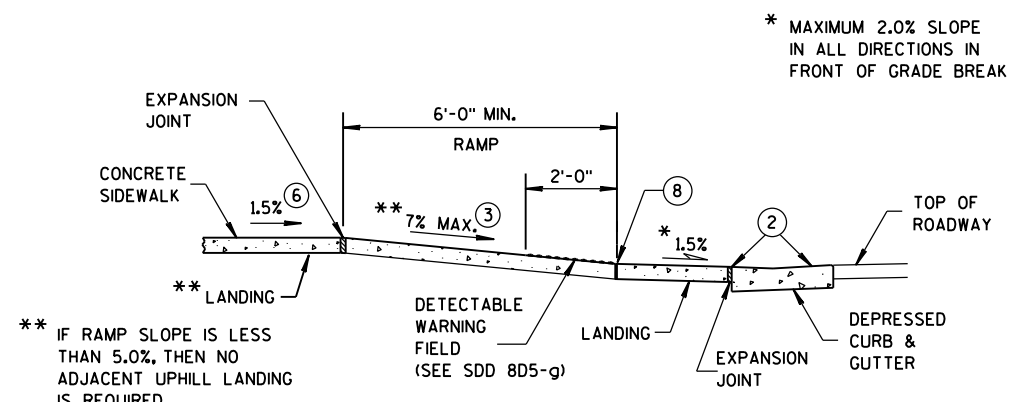
STATE OF WISCONSIN
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**CURB RAMP TYPE 4B
PLAN VIEW**



SECTION A-A FOR TYPE 4B



SECTION B-B FOR TYPE 4B

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

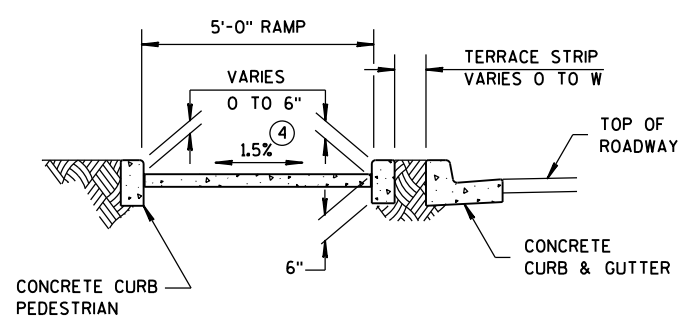
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2'-10 ¹ / ₄ "	0'-5"	2'-1"	1'-4 ¹ / ₂ "	1'-5"	2'-1"	0'-10"	2'-7 ¹ / ₂ "	0'-3 ¹ / ₄ "	3'-0 ¹ / ₄ "						
15 FEET	4'-6 ³ / ₄ "	2'-1 ³ / ₄ "	3'-9"	3'-5 ¹ / ₄ "	3'-1 ¹ / ₄ "	4'-6"	2'-6 ³ / ₄ "	5'-4 ¹ / ₂ "	2'-1"	6'-1"	1'-8"	6'-8 ¹ / ₂ "	1'-3 ¹ / ₄ "	7'-2 ¹ / ₂ "	0'-10 ³ / ₄ "	7'-7 ¹ / ₄ "
20 FEET	5'-9 ³ / ₄ "	3'-6 ¹ / ₂ "	4'-11 ¹ / ₂ "	5'-1 ³ / ₄ "	4'-3 ¹ / ₄ "	6'-5 ¹ / ₂ "	3'-8 ³ / ₄ "	7'-7"	3'-3"	8'-6 ¹ / ₂ "	2'-10"	9'-4 ¹ / ₂ "	2'-5 ¹ / ₂ "	10'-1 ¹ / ₄ "	2'-1 ¹ / ₄ "	10'-9"
30 FEET			6'-9 ¹ / ₄ "	7'-11 ¹ / ₄ "	6'-0 ¹ / ₄ "	9'-8"	5'-5"	11'-1 ³ / ₄ "	4'-10 ³ / ₄ "	12'-5 ³ / ₄ "	4'-5 ¹ / ₂ "	13'-7 ³ / ₄ "	4'-0 ³ / ₄ "	14'-8 ¹ / ₂ "	3'-8 ¹ / ₂ "	15'-8 ¹ / ₄ "
40 FEET									6'-1 ³ / ₄ "	15'-8 ¹ / ₂ "	5'-8"	17'-2"	5'-3"	18'-5 ³ / ₄ "	4'-10 ³ / ₄ "	19'-8 ¹ / ₄ "
50 FEET															5'-10 ¹ / ₄ "	23'-2"

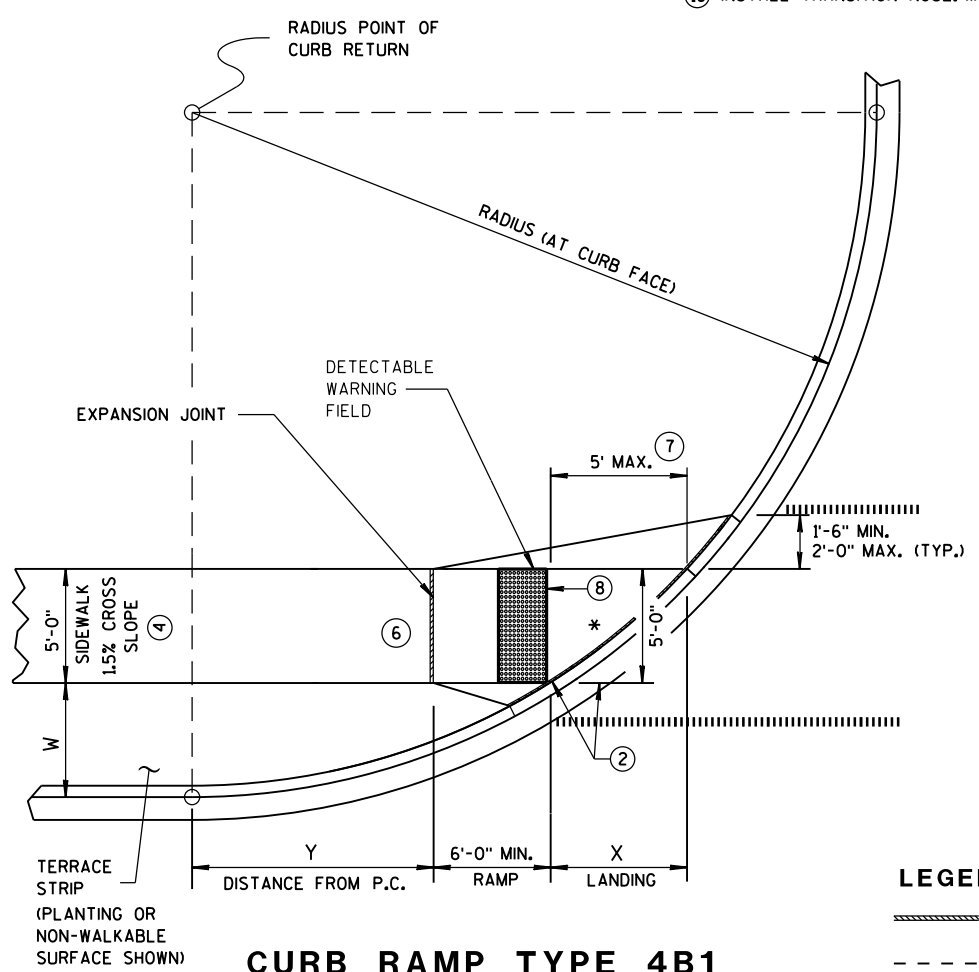
INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

GENERAL NOTES

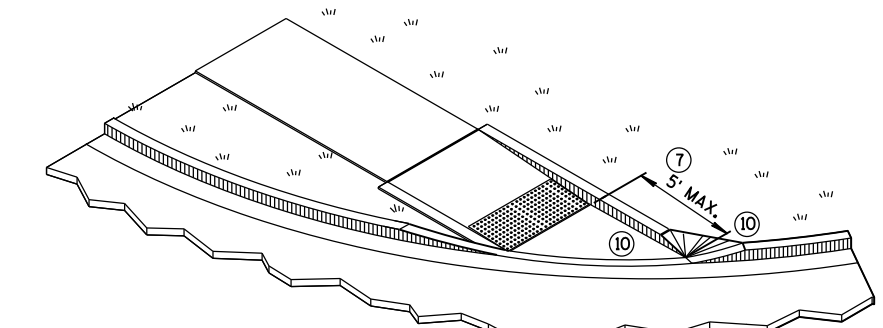
- 1. AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS. DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- 2. GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3. ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4. ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6. PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- 7. WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- 8. PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 10. INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



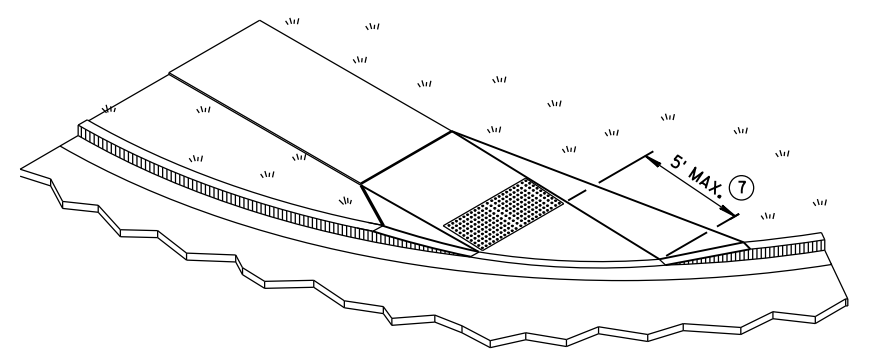
SECTION C-C FOR TYPE 4B



**CURB RAMP TYPE 4B1
PLAN VIEW**



ISOMETRIC VIEW FOR TYPE 4B

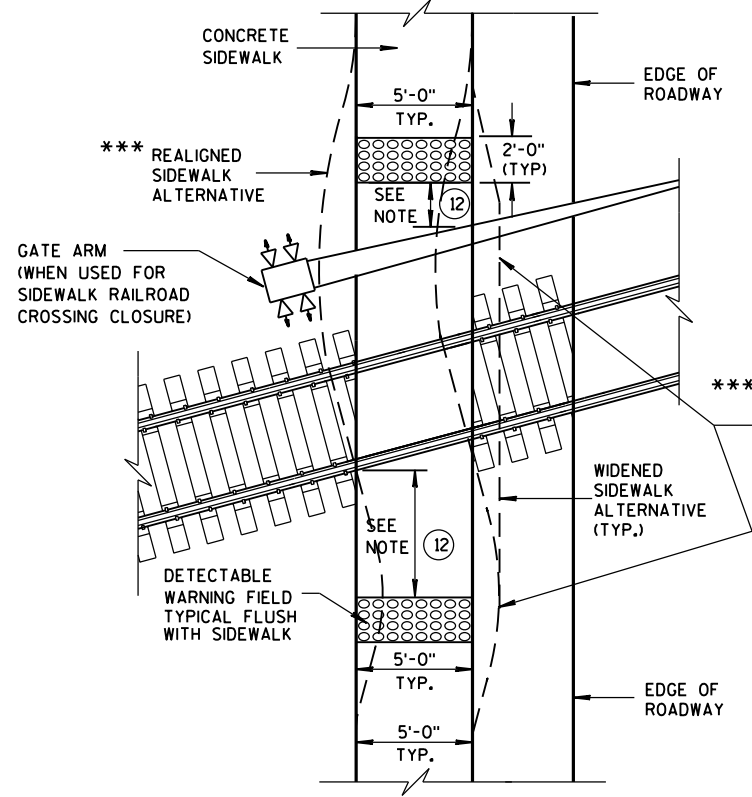


ISOMETRIC VIEW FOR TYPE 4B1

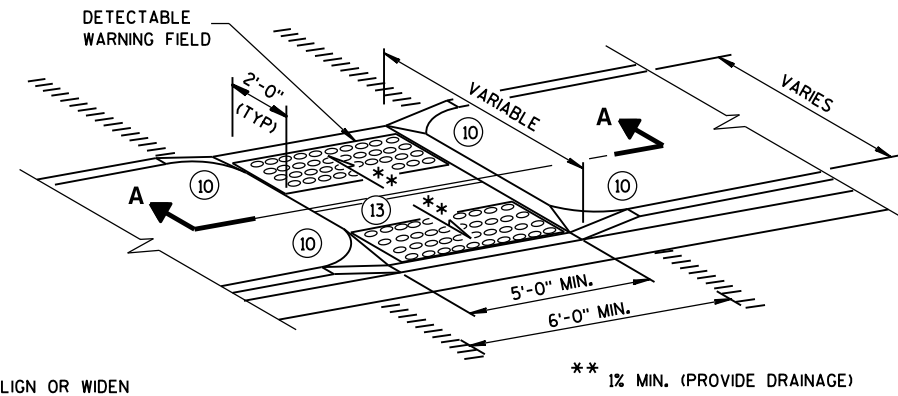
- LEGEND**
- 1/2" EXPANSION JOINT-SIDEWALK
 - - - - - CONTRACTION JOINT FIELD LOCATED
 - ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**TYPE 8
DETECTABLE WARNINGS
AT RAILROAD CROSSING**



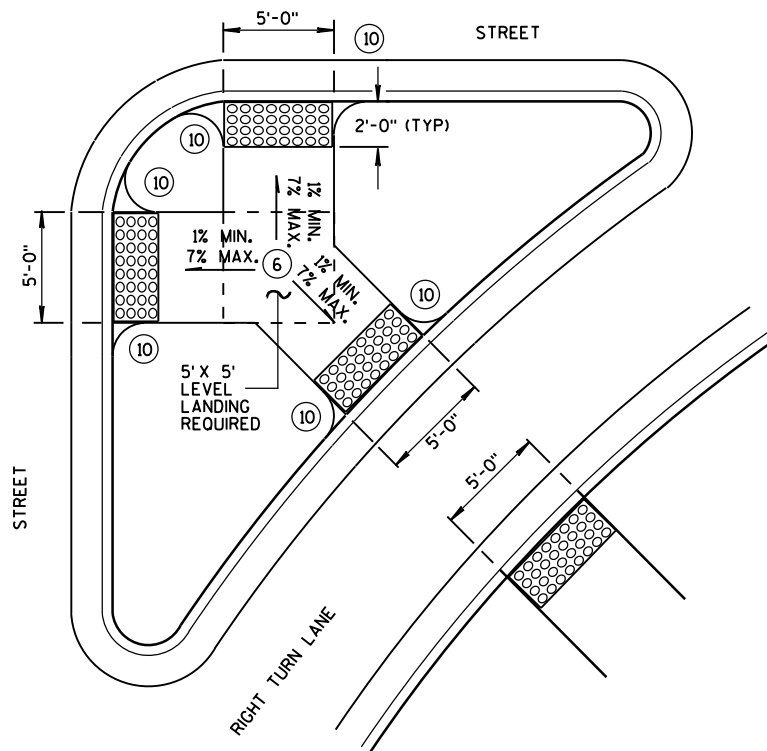
**MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING
TYPE 5**

*** DETAILS TO BE DETERMINED BY DESIGNER

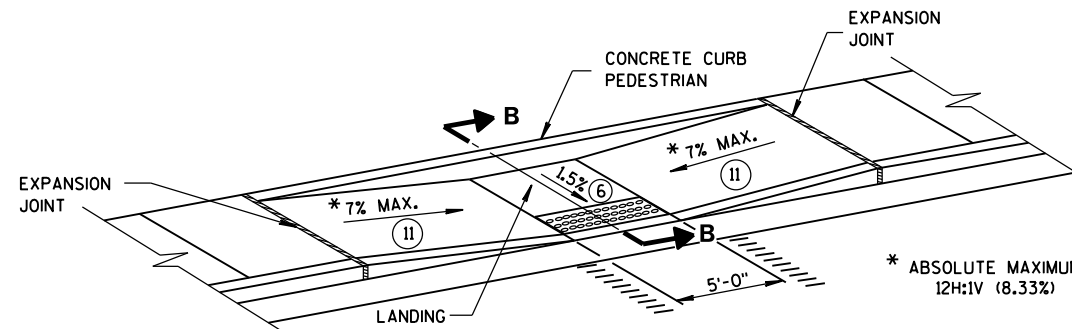
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.
- ⑩ INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 15 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2-FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

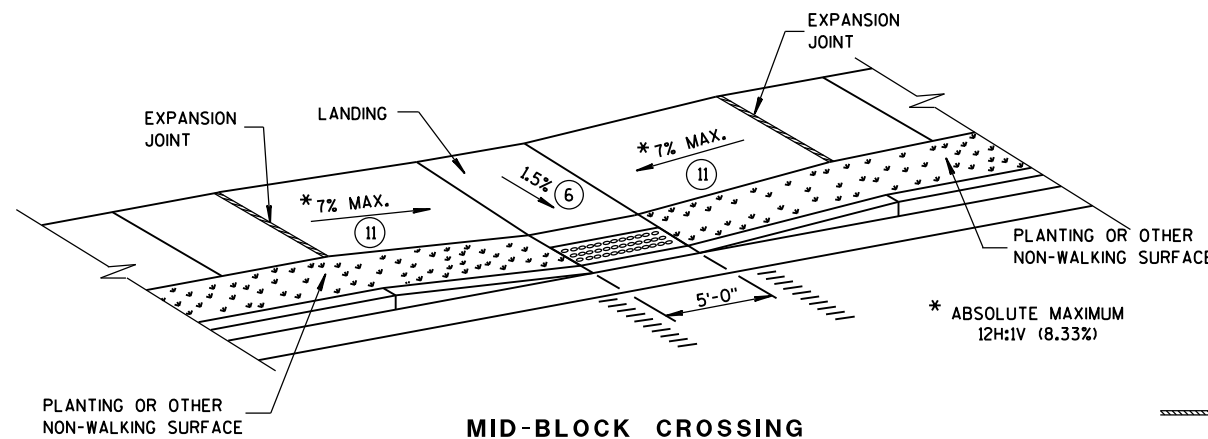
REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



**TYPE 6
DETECTABLE WARNING AT ISLANDS**

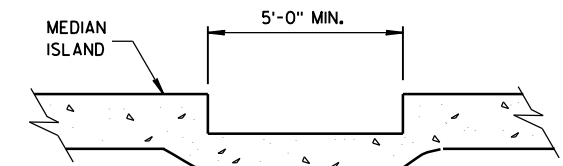


**MID-BLOCK CROSSING
TYPE 7A**

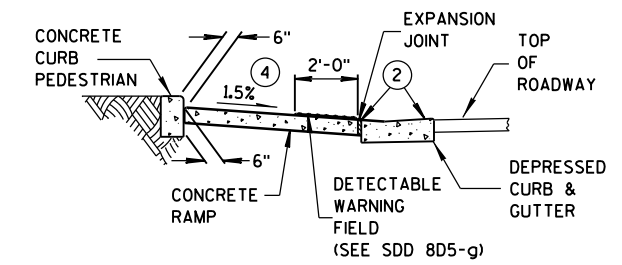


**MID-BLOCK CROSSING
TYPE 7B**

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.



SECTION A-A



SECTION B-B

LEGEND

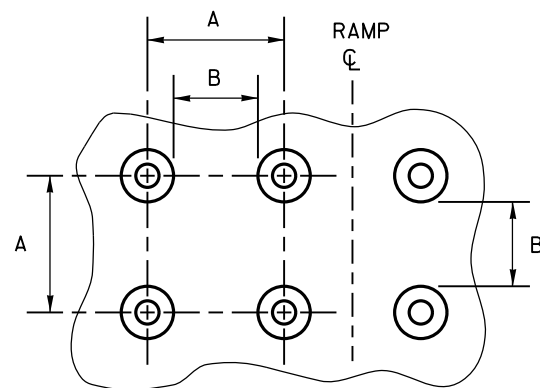
- 1/2" EXPANSION JOINT-SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPES 5, 6, 7A, 7B & 8**

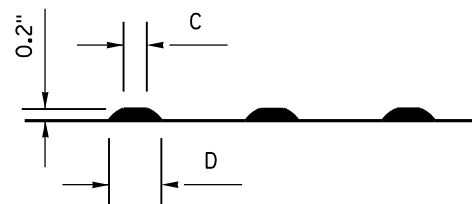
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

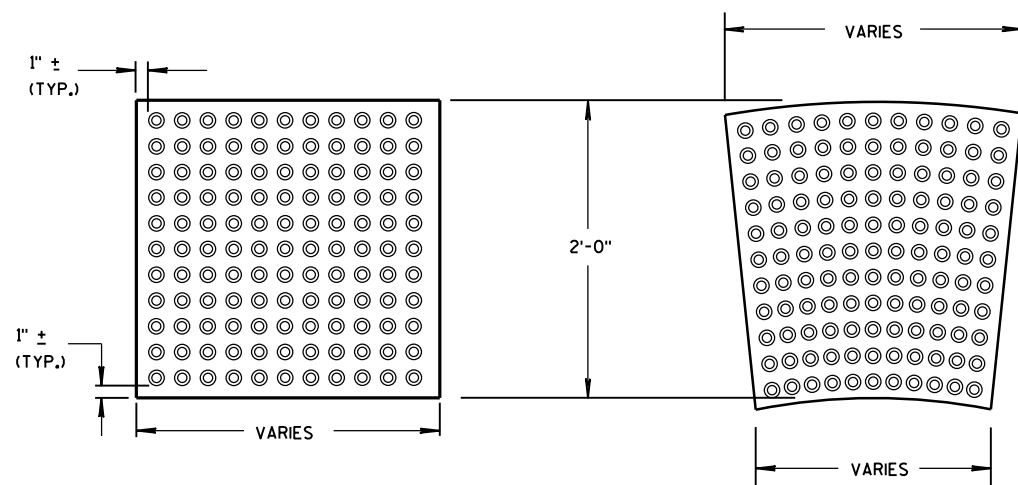


PLAN VIEW



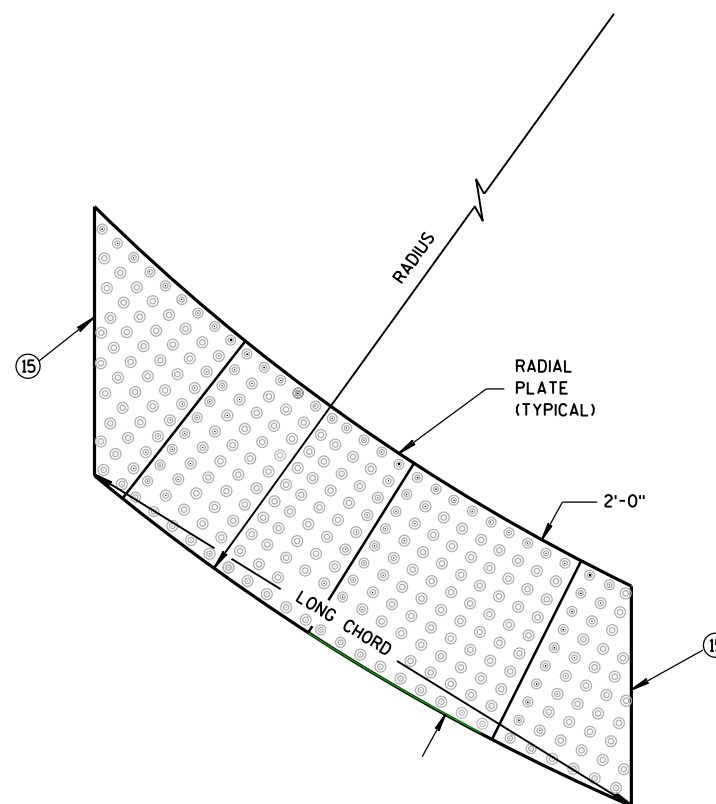
ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**



**RECTANGULAR PLATES
DETECTABLE WARNING FIELDS (TYPICAL)**

PLAN VIEW



**RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

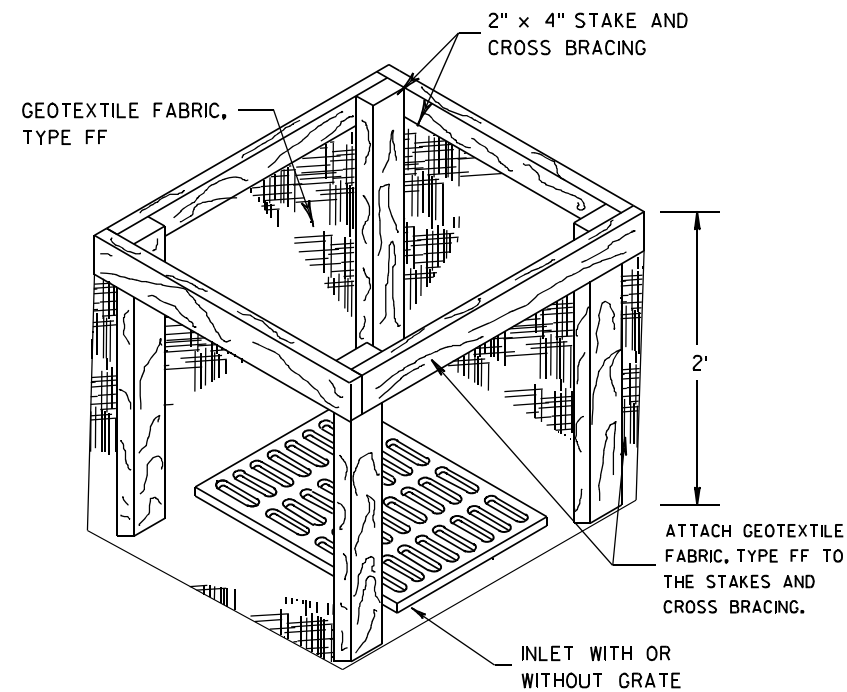
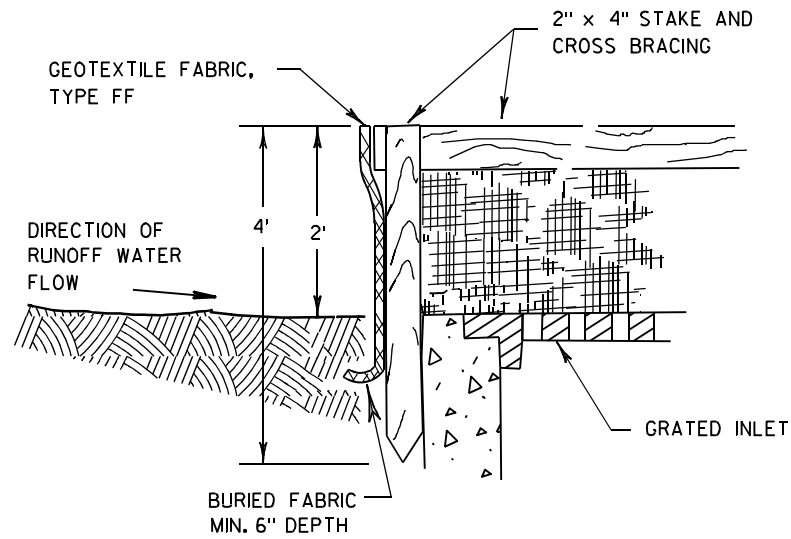
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGES IN COMBINATION WITH SQUARE PANELS ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

⑮ FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June, 2017 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR FHWA



INLET PROTECTION, TYPE A

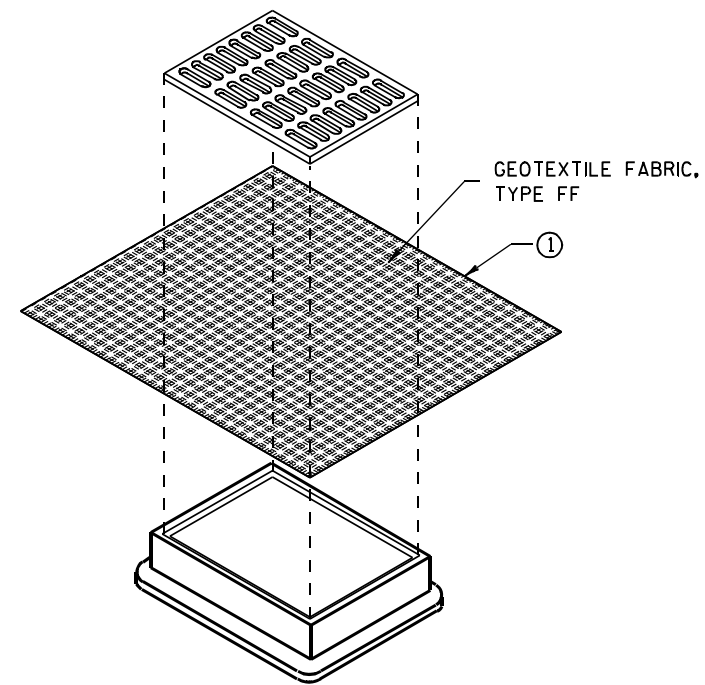
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

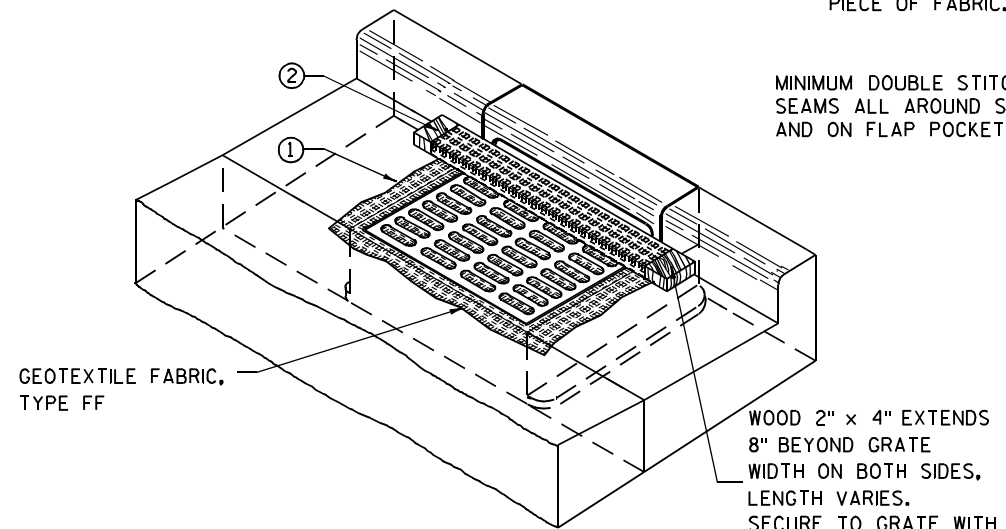
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

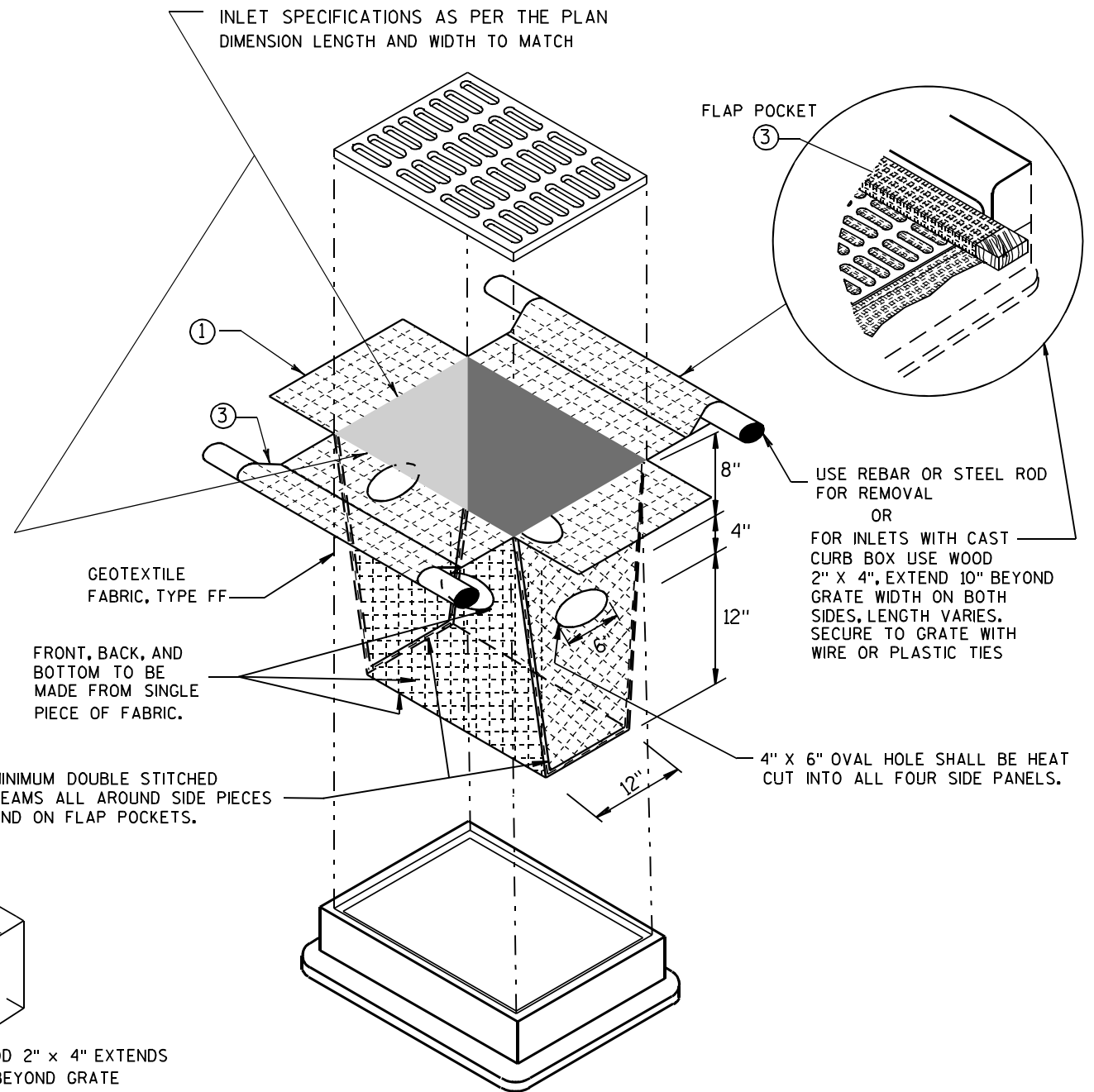
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

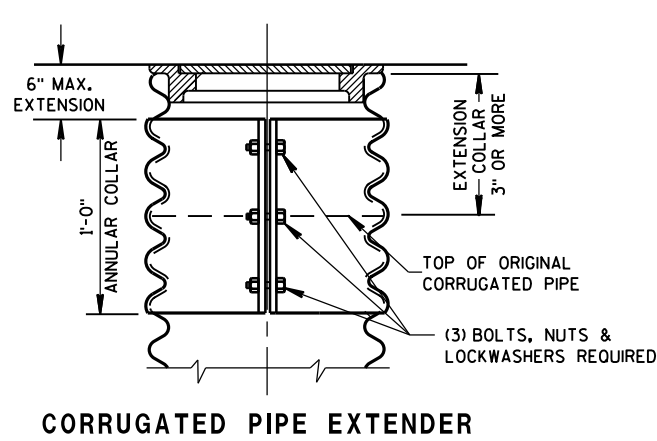
APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

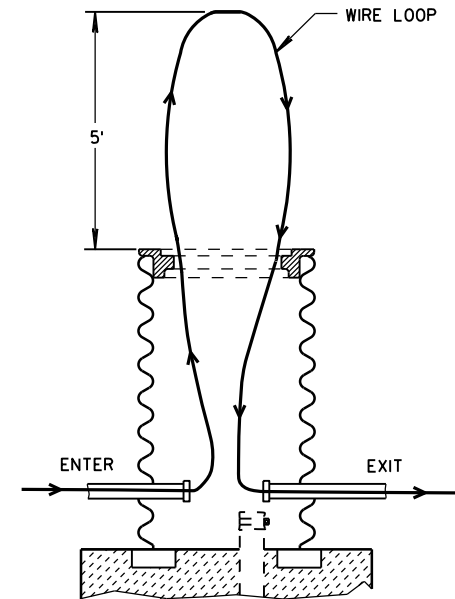
DIMENSION IN INCHES		CORRUGATED STEEL PIPE								
PIPE DIAMETER (INSIDE)	A	12	12	12	18	18	18	24	24	24
PIPE LENGTH **	B	24	30	36	24	30	36	36	42	48
WALL THICKNESS	C	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 1/4	10 1/4	10 1/4	16 1/4	16 1/4	16 1/4	22 1/4	22 1/4	22 1/4
FRAME	E	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2	26 1/2	26 1/2	26 1/2
FRAME	F	8 1/2	8 1/2	8 1/2	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2
FRAME	G	11 1/2	11 1/2	11 1/2	17 1/2	17 1/2	17 1/2	23 1/2	23 1/2	23 1/2
WEIGHT IN POUNDS *										
FRAME AND COVER		60	60	60	110	110	110	155	155	155

* THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.

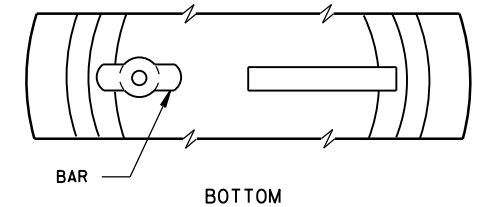
** NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.



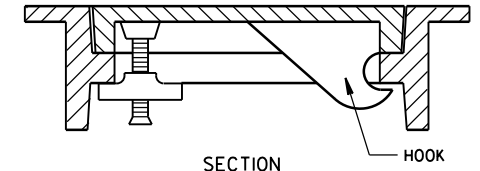
CORRUGATED PIPE EXTENDER



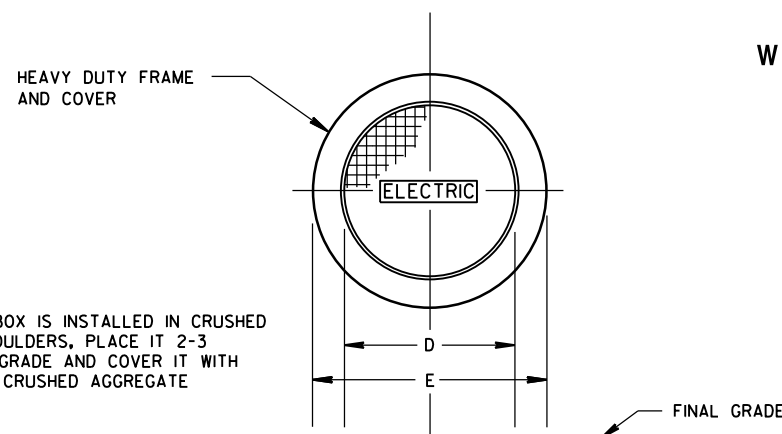
MEASUREMENT DETAIL FOR WIRE/CABLE IN THE PULL BOX



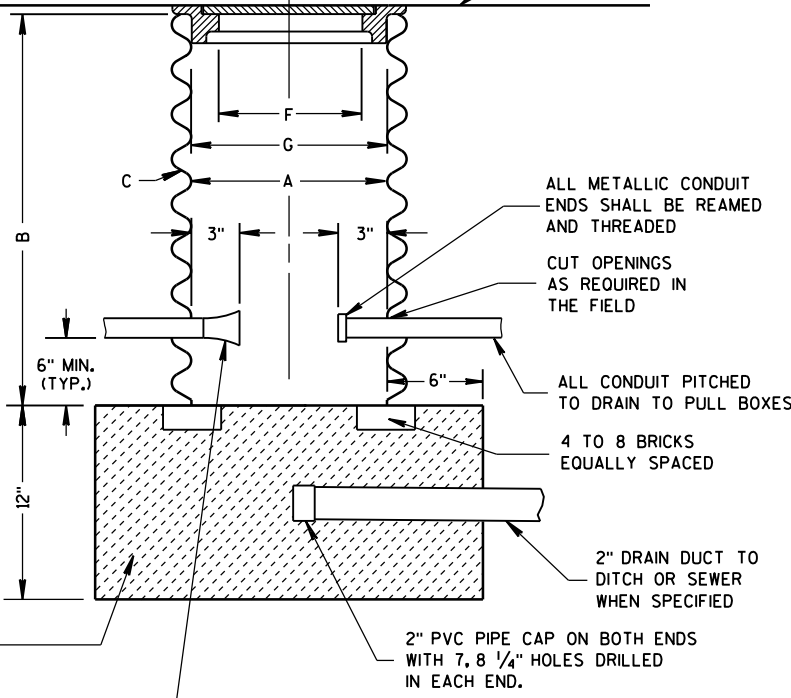
ALTERNATE COVER (LOCKING)



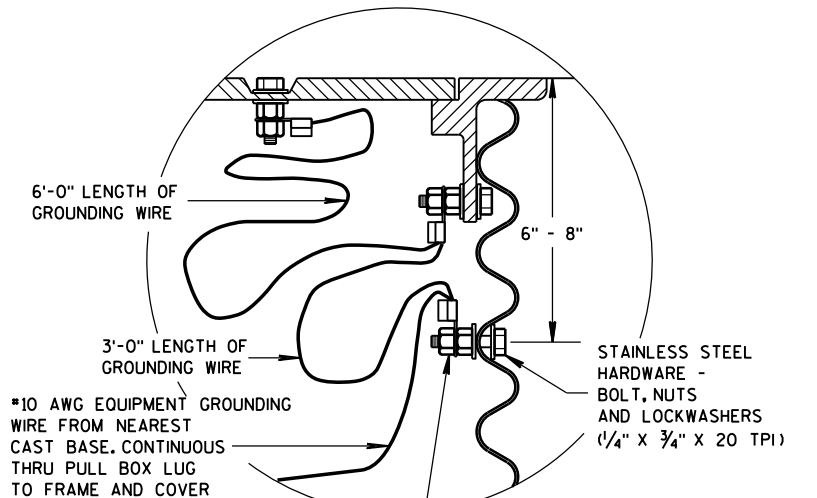
TIGHTENING BAR TYPE



WHEN A PULL BOX IS INSTALLED IN CRUSHED AGGREGATE SHOULDERS, PLACE IT 2-3 INCHES BELOW GRADE AND COVER IT WITH 2-3 INCHES OF CRUSHED AGGREGATE

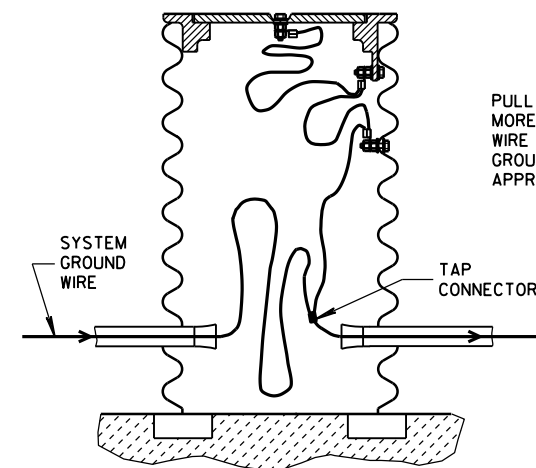


PULL BOX



NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE.

EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES



EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES

PULL BOX TO NEAREST BASE DISTANCE MORE THAN 20 FEET. PULL BOX GROUND WIRE SHALL CONNECT AT SYSTEM GROUNDING WIRE. USE DEPARTMENT APPROVED TAP CONNECTOR.

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR TRAFFIC LOADS.

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED FOR USE WITH COPPER WIRE.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

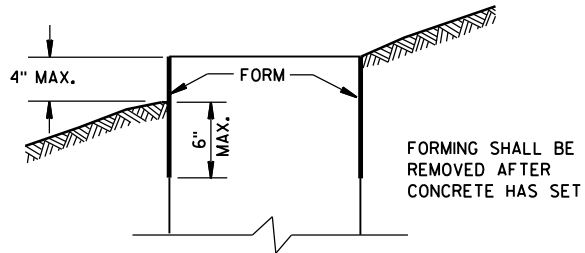
WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.

NO. 2 COARSE AGGREGATE (SEE SECTION 501 OF THE STANDARD SPECIFICATIONS)

INSTALL END BELLS (U.L. LISTED FOR ELECTRICAL USE) ON ALL NONMETALLIC CONDUIT BEFORE INSTALLATION OF WIRE AND/OR CABLE.

PULL BOX	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Ahmet Demirelek STATE ELECTRICAL ENGINEER
FHWA	

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS.

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN AT THE ENTRANCE OF THE BASE.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6 X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

GENERAL NOTES (CONTINUED)

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION OF CABLE OR WIRE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FOR FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC.

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG, STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 1, TYPE 2, TYPE 5, AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER THE BASE OF THE TYPE 2 AND TYPE 5 BASES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD, ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4" "L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH, THE "L" BEND END SHALL NOT BE THREADED.

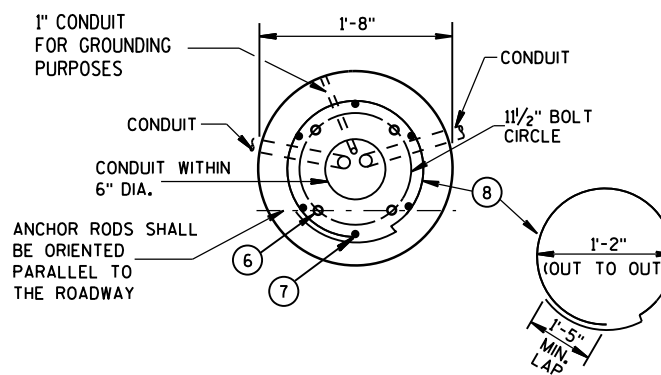
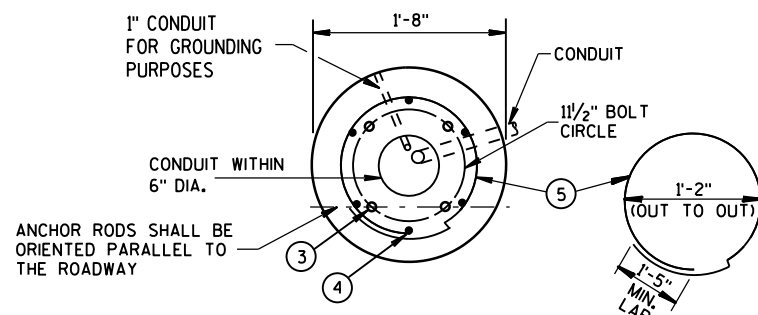
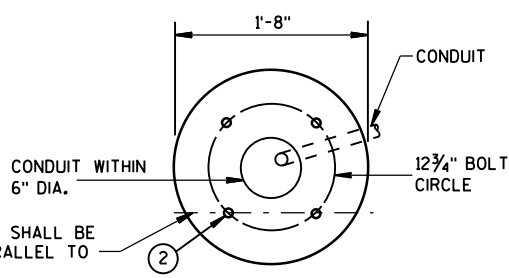
ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

1 THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL BY THE ENGINEER.

- 2 (4) 1" DIA. X 3'-6" ANCHOR RODS.
- 3 (4) 1" DIA. X 5'-0" ANCHOR RODS.
- 4 (6) NO. 6 X 6'-8" BAR STEEL REINFORCEMENT.
- 5 (7) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.
- 6 (4) 1" DIA. X 3'-6" ANCHOR RODS.
- 7 (6) NO. 4 X 4'-8" BAR STEEL REINFORCEMENT.
- 8 (5) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.



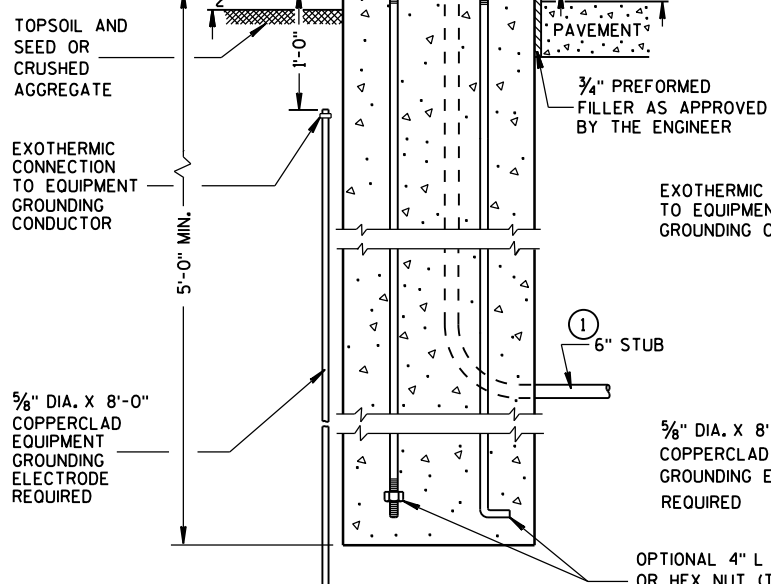
FORM ALL EXPOSED CONCRETE. PROVIDE 1" CHAMFER ALL AROUND

6 HALF SECTION IN UNPAVED AREA (TYPICAL FOR TYPES 1, 2, 5, & 6)

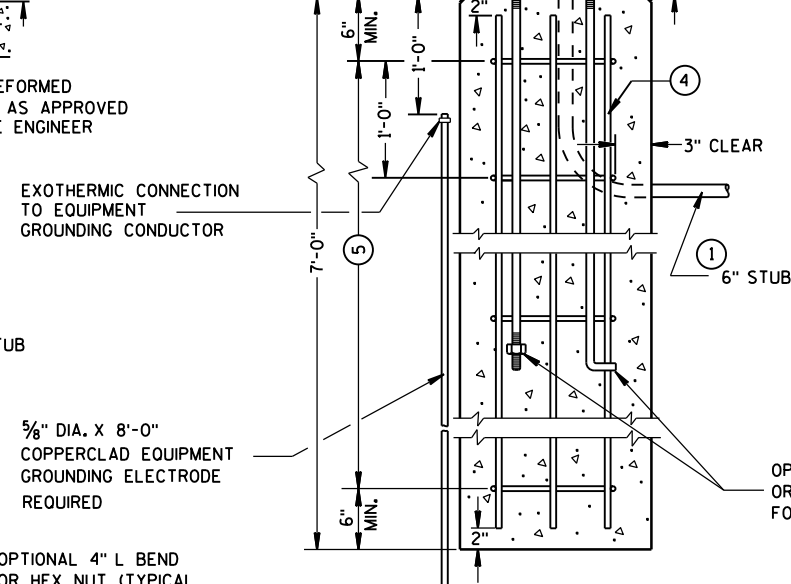
HALF SECTION IN PAVEMENT (TYPICAL FOR TYPES 1, 2, 5, & 6)

FORM ALL EXPOSED CONCRETE. PROVIDE 1" CHAMFER ALL AROUND

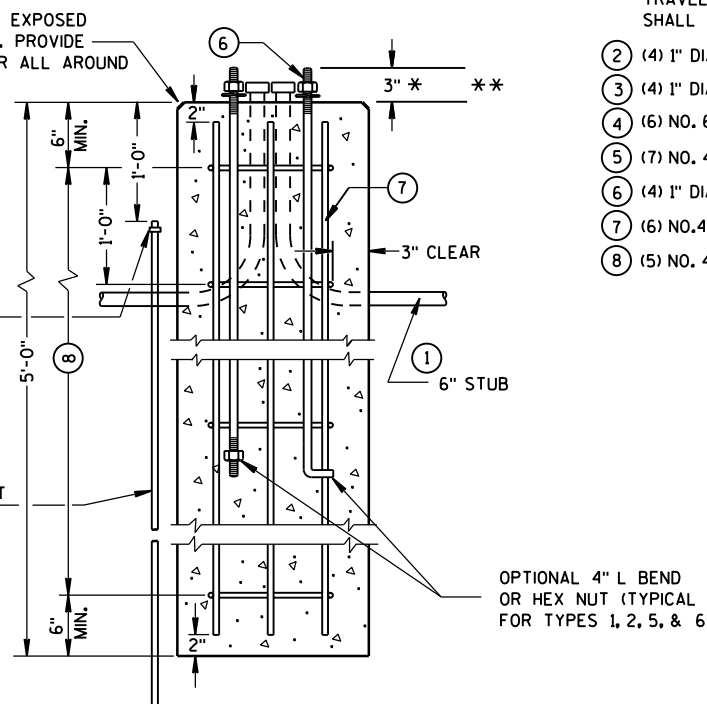
FORM ALL EXPOSED CONCRETE. PROVIDE 1" CHAMFER ALL AROUND



TYPE 1



TYPE 2



TYPE 5 & 6

CONCRETE BASES

* ANY ANCHOR ROD PROJECTION SHORTER THAN 2 3/4" OR LONGER THAN 3 1/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

** FOR NONBREAKAWAY INSTALLATIONS, 4 1/2" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

CONCRETE BASES, TYPES 1, 2, 5, & 6

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sep. 2014 /S/ Ahmet Demireblek
DATE STATE ELECTRICAL ENGINEER
FHWA

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

FOUR (4) BOLTS SHALL BE FURNISHED WITH EACH TRANSFORMER BASE. BOLTS SHALL BE 1" DIAMETER, 4" IN LENGTH, WITH WASHERS, LOCK WASHERS AND NUTS. BOLTS, NUTS AND WASHERS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 641.2.2 OF THE STANDARD SPECIFICATIONS.

LEVELING SHIMS, IF NEEDED, SHALL BE DESIGNED FOR THE PURPOSE AND USED UNDER CAST BASES WHEN PLUMBING POLES OR STANDARDS DURING INSTALLATION. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE.

SHIM LENGTH SHALL BE LONG ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.

DOUBLE NUTTING IS NOT ACCEPTABLE FOR LEVELING OR MOUNTING PURPOSES.

A NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE SHALL BE FURNISHED AND INSTALLED IN THE PEDESTAL AND TRANSFORMER BASES.

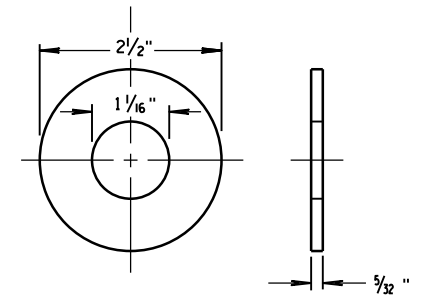
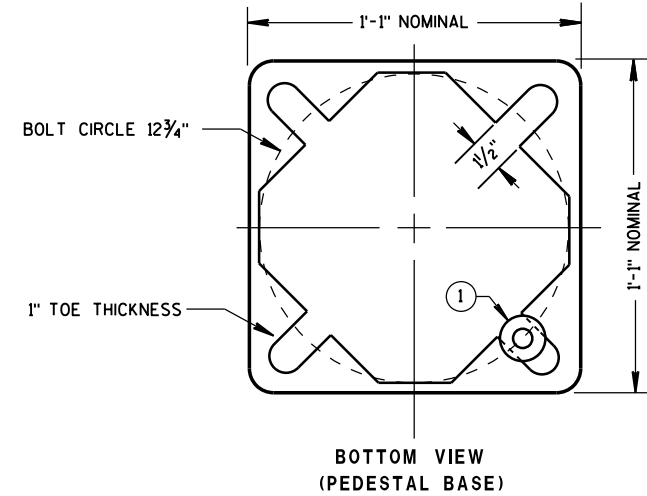
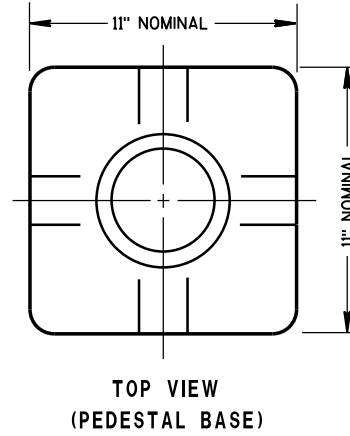
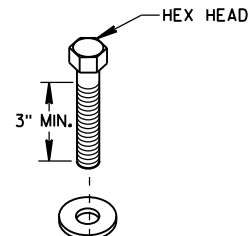
THE MECHANICAL CONNECTOR SHALL BE INSTALLED USING A 1/4" - 20 (TPI) STAINLESS STEEL HEX HEAD BOLT OF SUFFICIENT LENGTH TO FIRMLY ATTACH THE LUG TO THE BASE.

SHOULD THE MANNER OF ATTACHMENT OF THE LUG REQUIRE WASHERS, HEX NUTS, LOCK WASHER - THEY SHALL BE STAINLESS STEEL AS IS THE BOLT. THE MANNER OF ATTACHMENT SHALL NOT BLOCK ACCESSIBILITY TO WIRE PLACEMENT IN THE CONNECTOR.

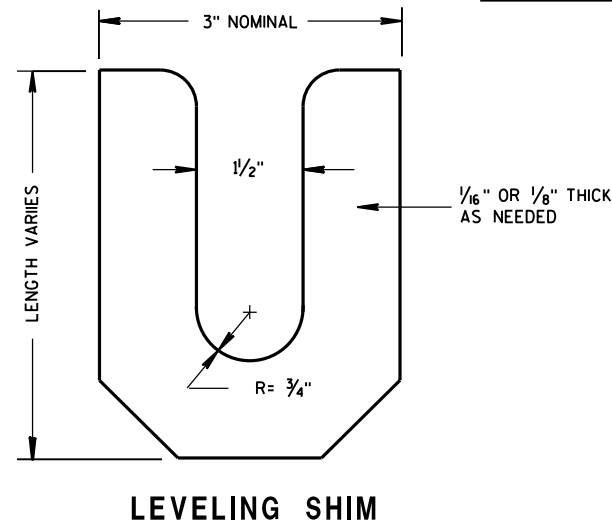
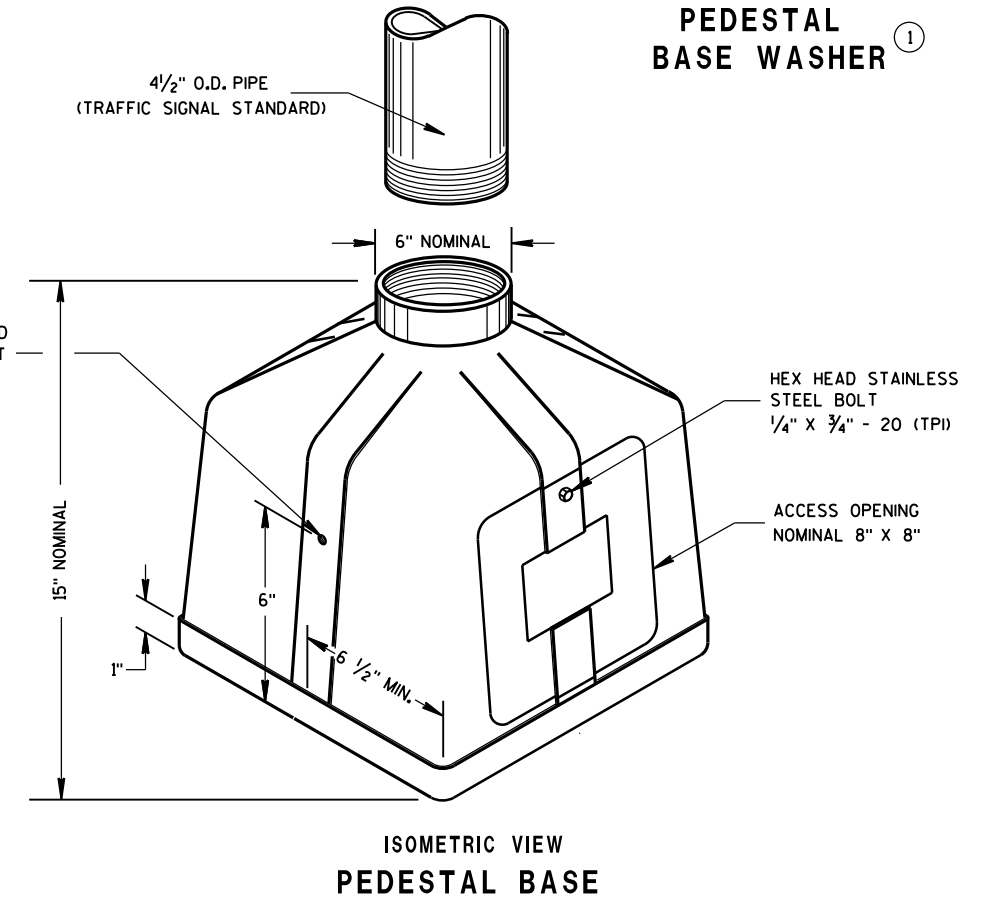
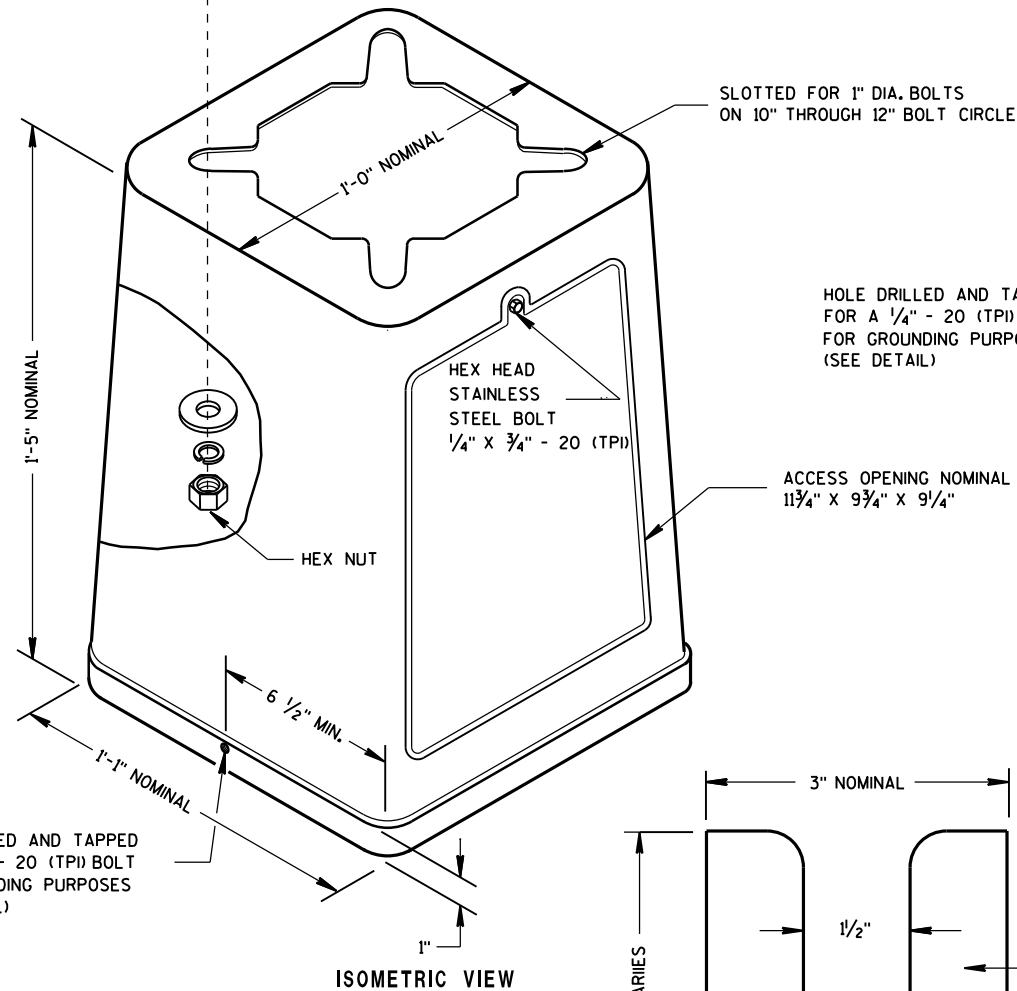
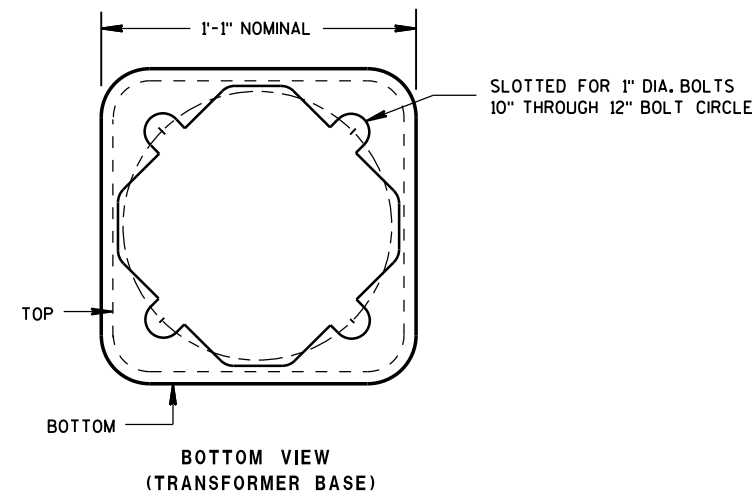
PEDESTAL BASE COLLAR THREADING SHALL BE TAPERED AND IN ACCORDANCE WITH NATIONAL PIPE THREADING DIMENSIONS.

BASE COLLAR THREADING SHALL EXTEND INTO THE BASE COLLAR WITH SUFFICIENT DEPTH TO ACCEPT THE INSTALLATION OF TRAFFIC SIGNAL STANDARDS TO A DEPTH OF 1/2", THEN TIGHTENING TO A POINT OF BEING IMMOVABLE.

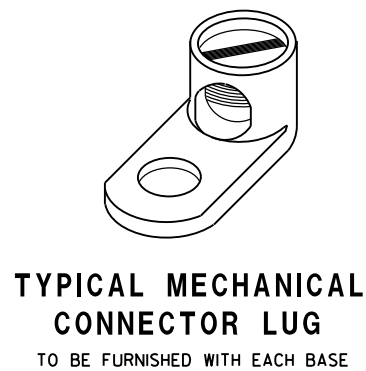
THE ACCESS DOOR SHALL BE OF THE SAME MATERIAL AS THE BASE.



ZINC COATED STEEL WASHER TO BE PROVIDED BY THE CONTRACTOR
PEDESTAL BASE WASHER ①



TRANSFORMER BASE
INTENDED FOR USE WITH TYPE 2, 3, 4, 5 & 6 POLES



TRANSFORMER/PEDESTAL BASES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED Sept. 2014 DATE	/S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER
FHWA	

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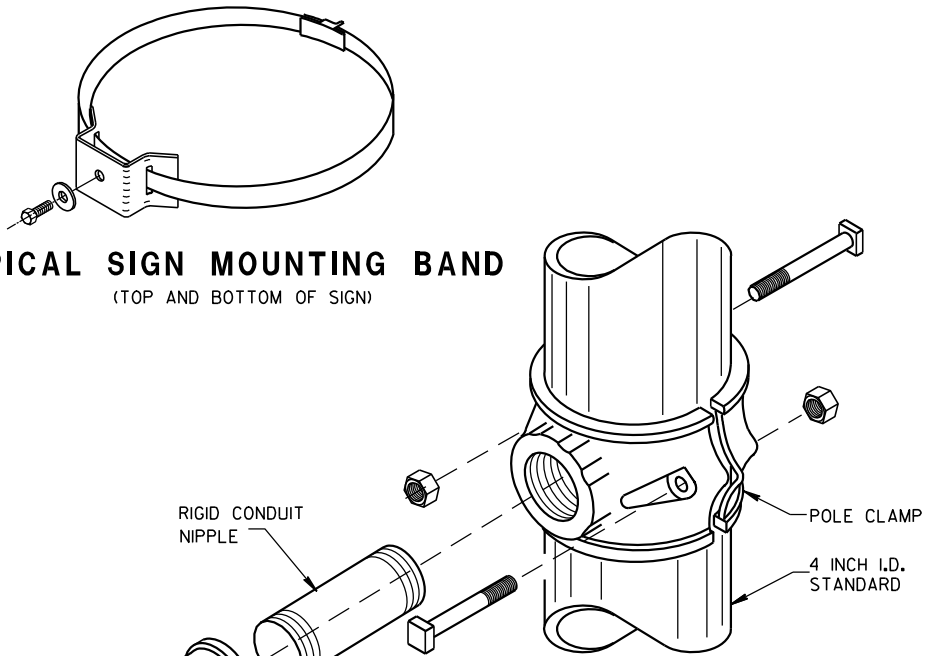
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S.D.D. 9 C 3-4

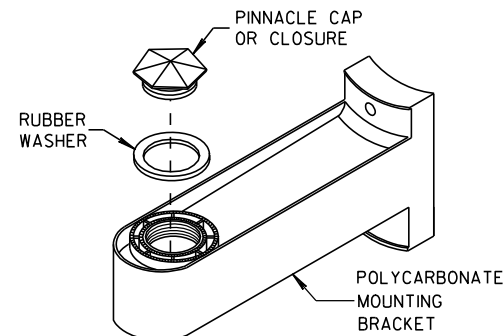
S.D.D. 9 C 3-4

TYPICAL SIGN MOUNTING BAND

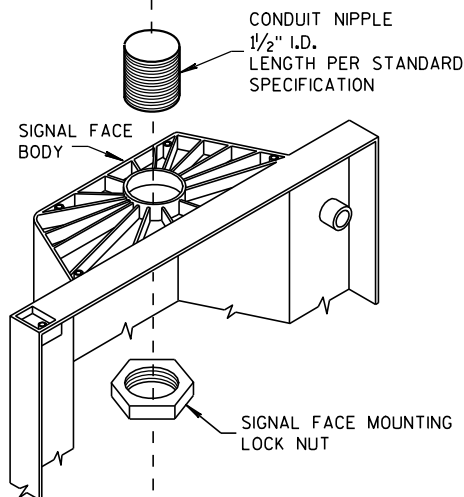
(TOP AND BOTTOM OF SIGN)



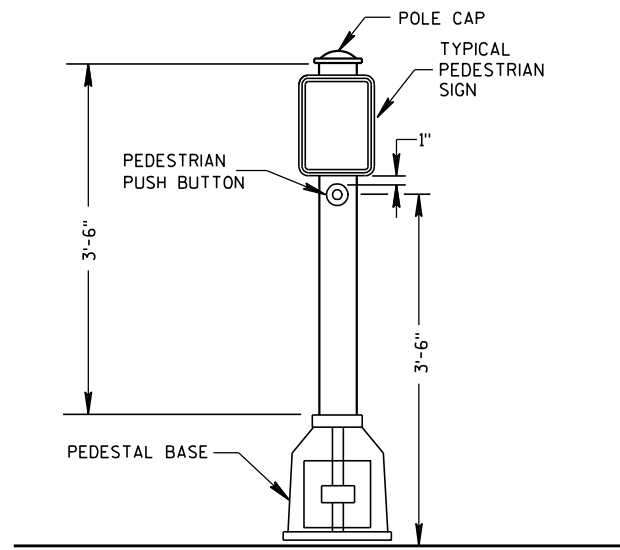
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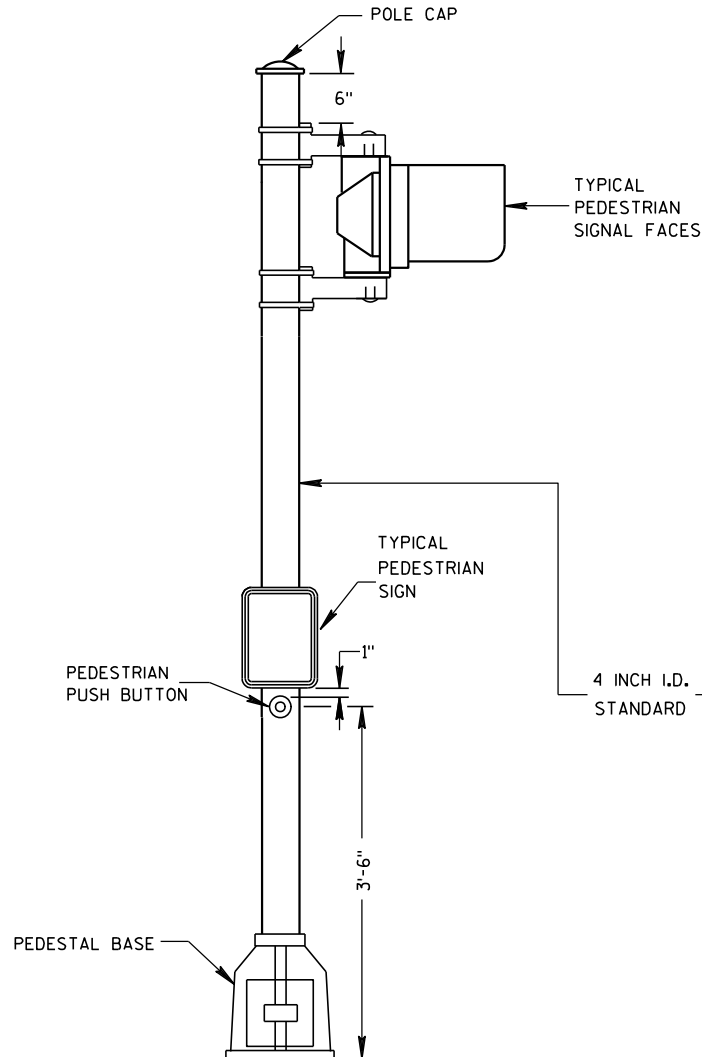
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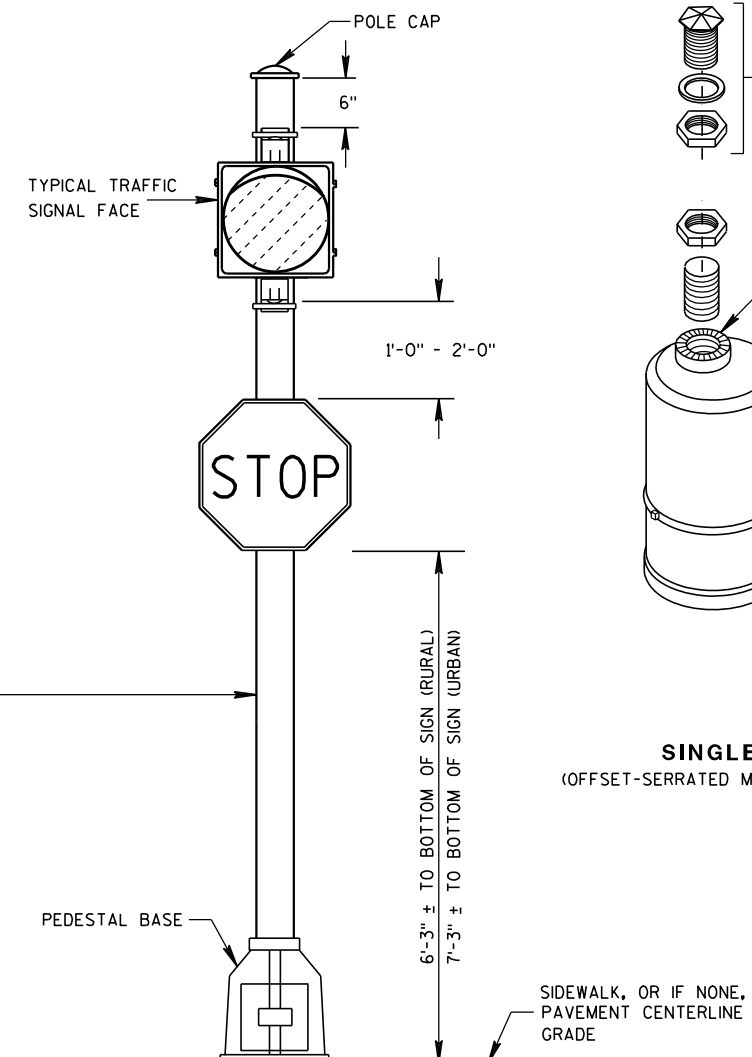
SIGNAL FACE MOUNTING DETAILS



**PEDESTRIAN PUSH BUTTON
TYPICAL MOUNTING**



**PEDESTRIAN FACE STANDARD-10 FT.
(WALK-DON'T WALK)**



**STANDARD FLASHER.
10 FOOT, 13 FOOT OR 15 FOOT AS REQUIRED**

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SEE THE SIGNAL PLAN FOR REQUIRED SIGNAL FACE SIZES.

LOCATIONS SHALL BE AS SHOWN ON THE PLANS.

ALL PEDESTAL BASES SHALL BE MOUNTED ON CONCRETE BASE - TYPE 1.

FOR APPROVED MOUNTING HARDWARE, SEE THE CONTRACT SPECIFICATIONS.

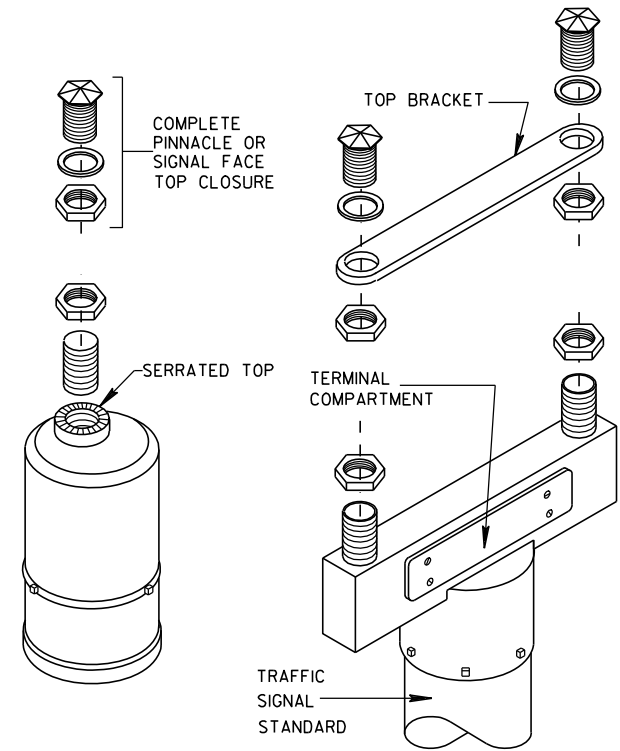
POLYCARBONATE SIGNAL FACE MOUNTING BRACKETS SHALL BE USED UNLESS ORNAMENTAL POLE CLAMPS ARE SPECIFIED.

LENGTH OF TRAFFIC STANDARDS SHALL BE AS SHOWN ON THE PLANS.

MOUNTINGS AND BRACKETS SHALL BE AS SHOWN ON THE PLANS OR DESCRIBED IN THE SPECIAL PROVISIONS (BY THE DISTRICT TRAFFIC ENGINEER).

PEDESTRIAN SIGNS SHALL BE AS DESIGNATED IN THE PLANS.

FURNISH AND INSTALL VENTILATED, CAST, METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) 1/4" X 3/4" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.



SINGLE
(OFFSET-SERRATED MOUNTING)

DOUBLE
(SERRATED MOUNTING)

SLIPFITTERS

**TRAFFIC SIGNAL STANDARD
PEDESTRIAN AND FLASHER
TYPICAL MOUNTING DETAILS**

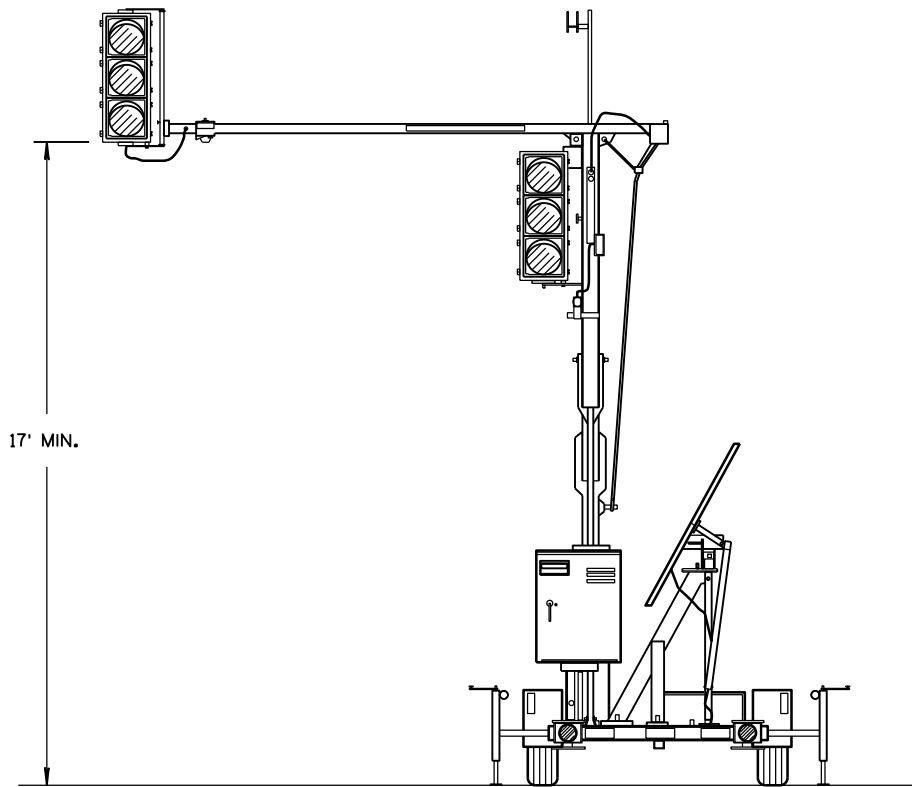
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5/11/10
DATE

/s/ John Corbin
STATE ELECTRICAL ENGINEER FOR HWYS

FHWA

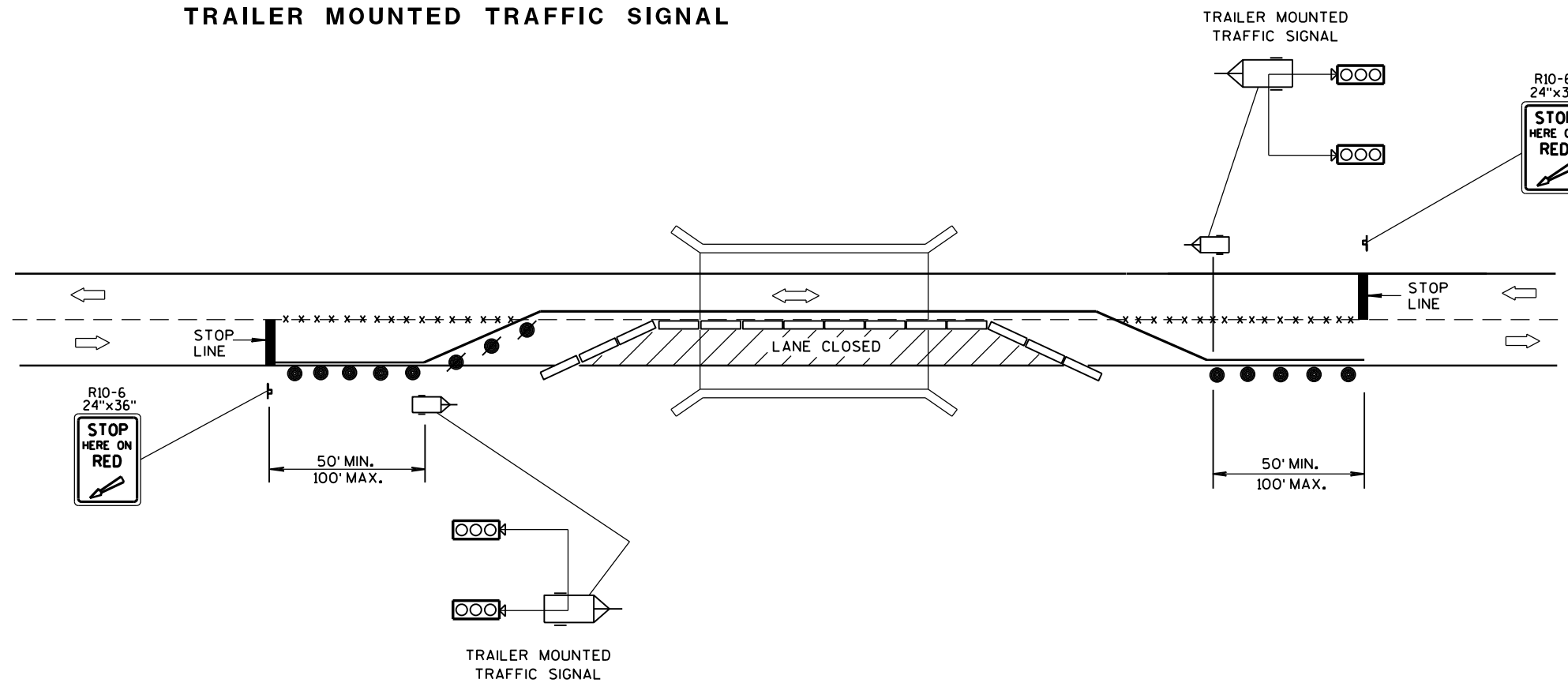


TRAILER MOUNTED TRAFFIC SIGNAL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15 D 33.



TYPICAL TRAILER MOUNTED TRAFFIC SIGNAL LOCATION

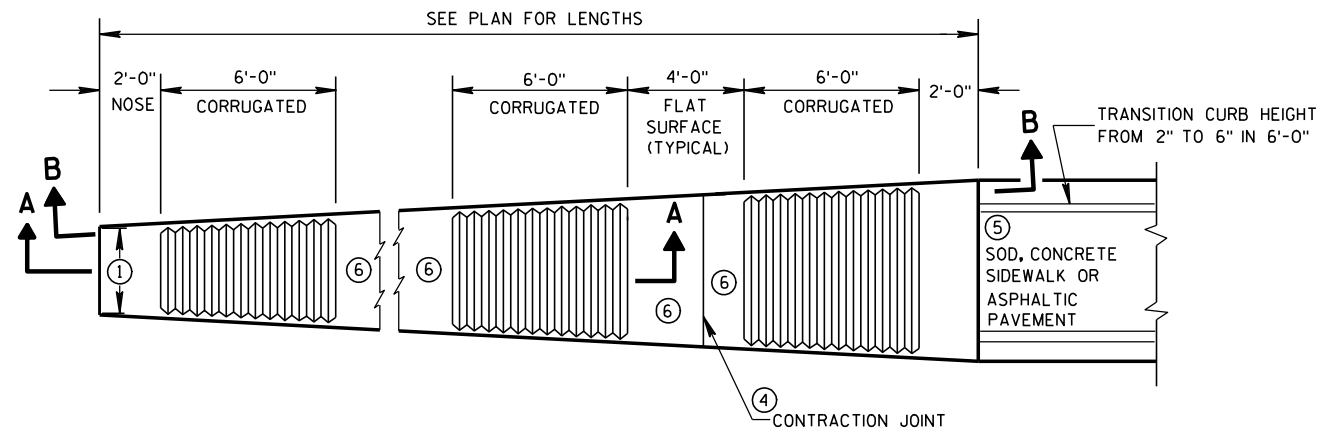
LEGEND

- ⌋ POST MOUNTED SIGN
- *-x-* REMOVING PAVEMENT MARKING
- /● DRUM WITH/WITHOUT WARNING LIGHT, TYPE C (STEADY-BURN)
- ▭ TEMPORARY PRECAST CONCRETE BARRIER
- ⌋ TRAILER MOUNTED TRAFFIC SIGNAL
- ➡ DIRECTION OF TRAFFIC FLOW

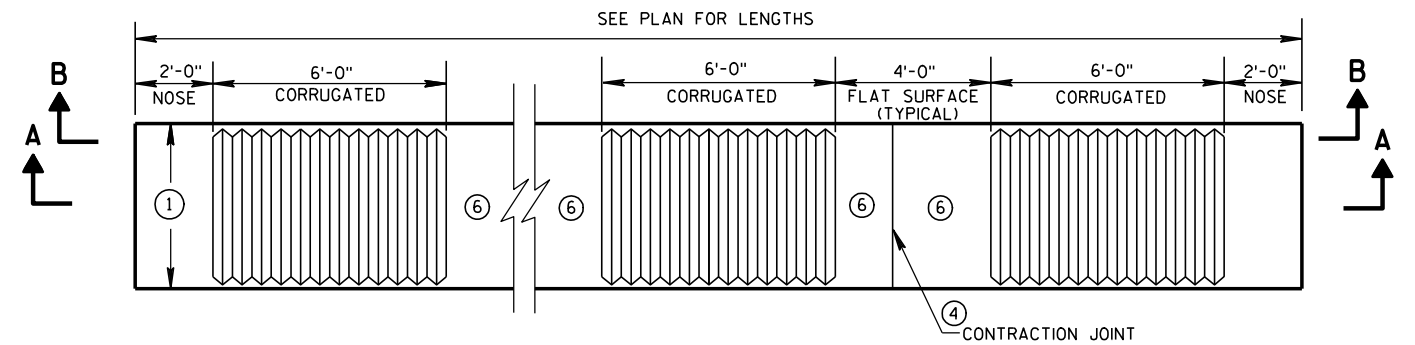
BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

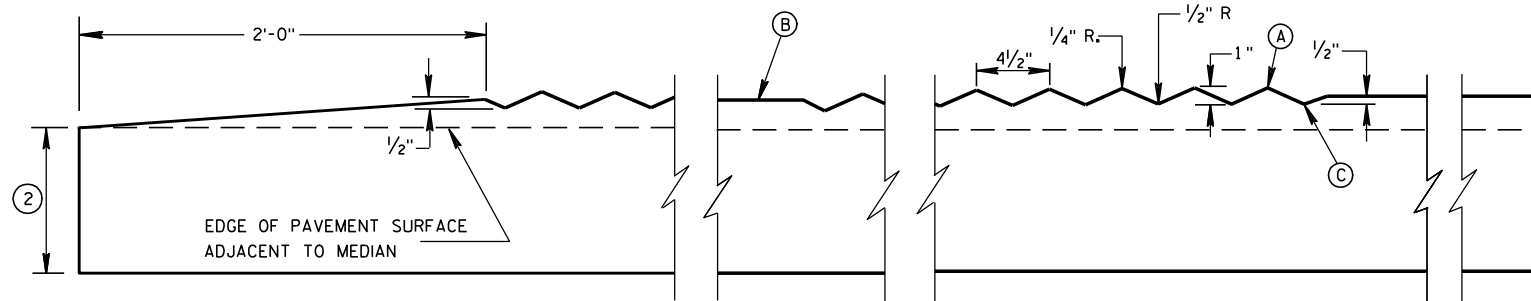
APPROVED
DATE: Sep 1, 2016 /S/ Ahmet Demirelek
STATE ELECTRICAL ENGINEER
FHWA



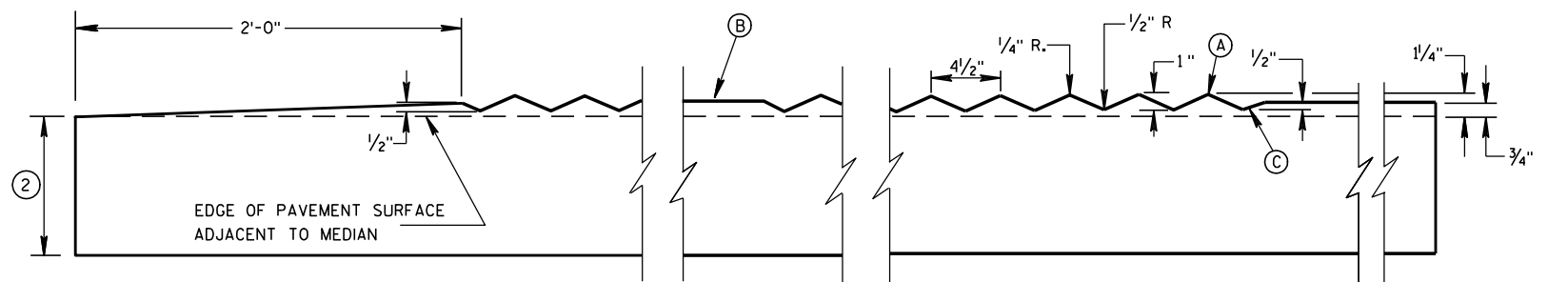
PLAN VIEW
VARIABLE WIDTH CONCRETE CORRUGATED MEDIAN



PLAN VIEW
UNIFORM WIDTH CONCRETE CORRUGATED MEDIAN



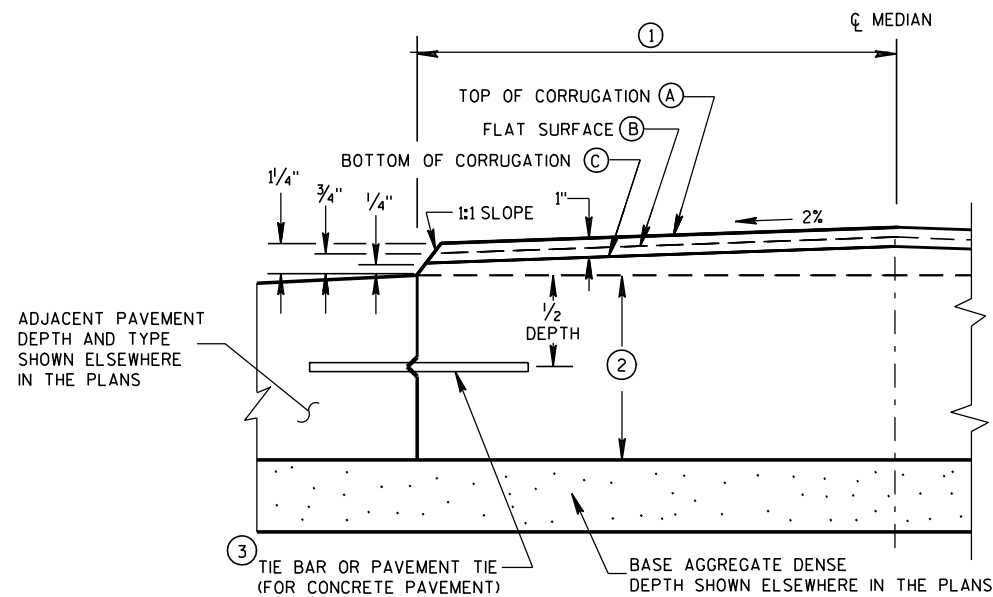
SECTION A-A
LONGITUDINAL SECTION



SECTION B-B
LONGITUDINAL SECTION

GENERAL NOTES

- ① SEE PLANS FOR CONSTANT OR VARIABLE WIDTH.
- ② THE DEPTH OF THE CONCRETE CORRUGATED MEDIAN SHALL BE 9-INCHES UNLESS SHOWN OTHERWISE IN THE PLAN. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN IN THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE OVER NEW OR EXISTING CONCRETE BASE COURSE, OR PAVEMENT.
 - (3) ASPHALTIC PAVEMENT OVER BASE AGGREGATE DENSE.
- ③ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C. INSTALL TIE BARS TO MAINTAIN A MINIMUM OF 3-INCHES OF COVER BETWEEN THE TIE BAR AND THE CONCRETE SURFACE (BOTTOM AND TOP). PAVEMENT TIES REQUIRED IN EXISTING CONCRETE PAVEMENT OR CONCRETE BASE COURSE, PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.
- ④ CONCRETE CORRUGATED MEDIAN CONTRACTION JOINTS SHALL BE CONSTRUCTED TO MATCH THE JOINTS IN ADJACENT CONCRETE PAVEMENT. WHERE ADJACENT PAVEMENT IS ASPHALT WITH BASE AGGREGATE DENSE, TRANSVERSE CONTRACTION JOINTS SHALL BE PROVIDED AT 20 FOOT INTERVALS.
- ⑤ SURFACE TYPE AND DETAILS ARE DEFINED ELSEWHERE IN THE PLAN.
- ⑥ YELLOW MARKING ON FLAT SURFACE WHEN MEDIAN SEPARATES OPPOSING TRAFFIC.

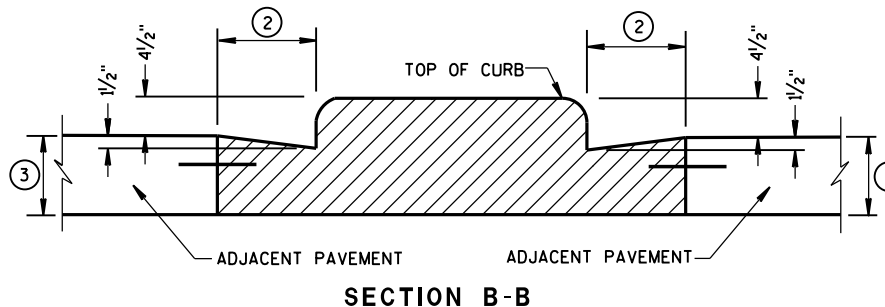
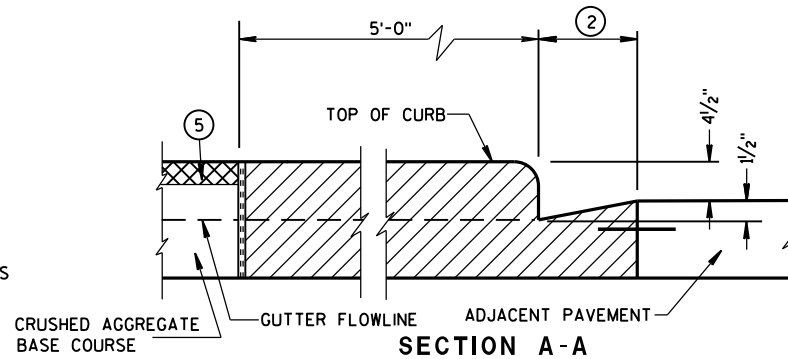
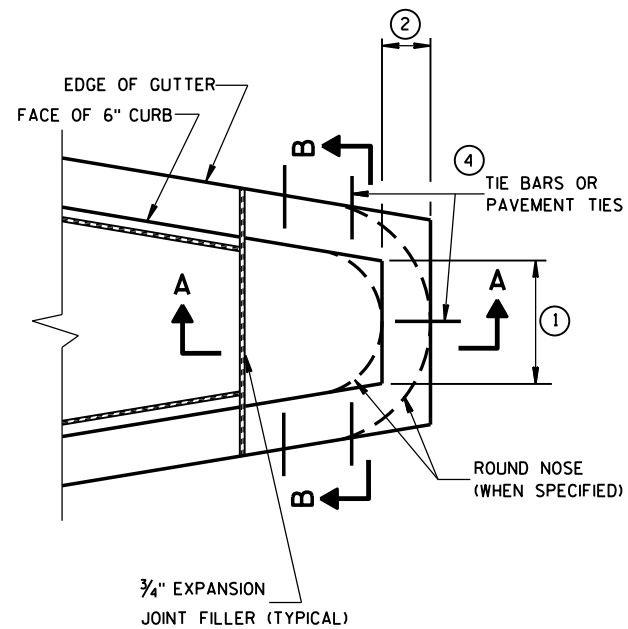
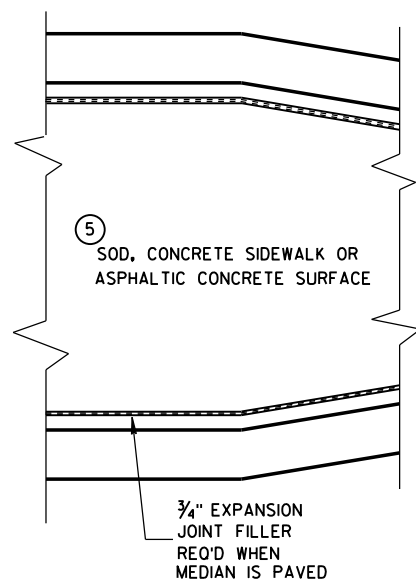


HALF CROSS SECTION
② CONCRETE CORRUGATED MEDIAN AND ADJACENT PAVEMENT

CONCRETE CORRUGATED MEDIAN

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
12/17/07 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

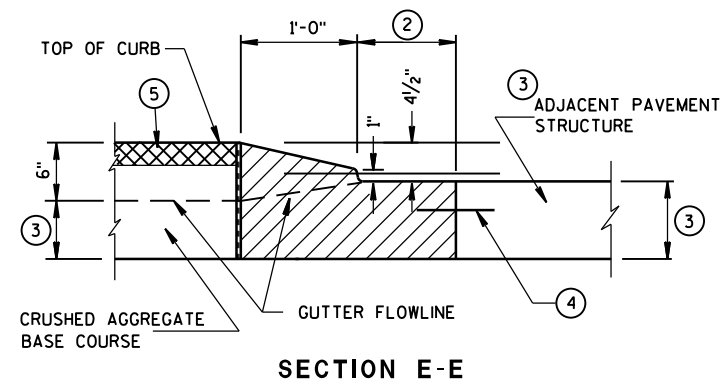
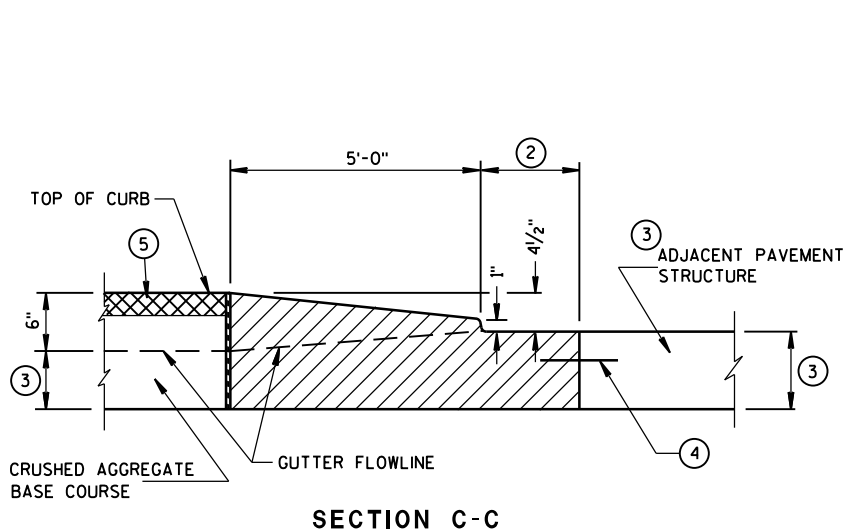
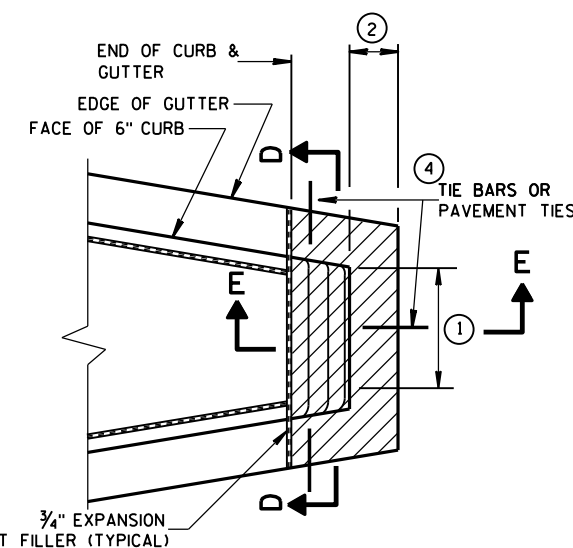


CONCRETE MEDIAN BLUNT NOSE DETAIL

GENERAL NOTES

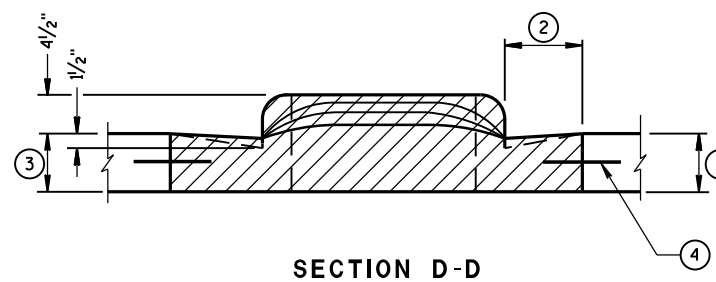
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.
- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.
- PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.
- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.



CONCRETE MEDIAN SLOPED NOSE TYPE 2

CONCRETE MEDIAN SLOPED NOSE TYPE 1



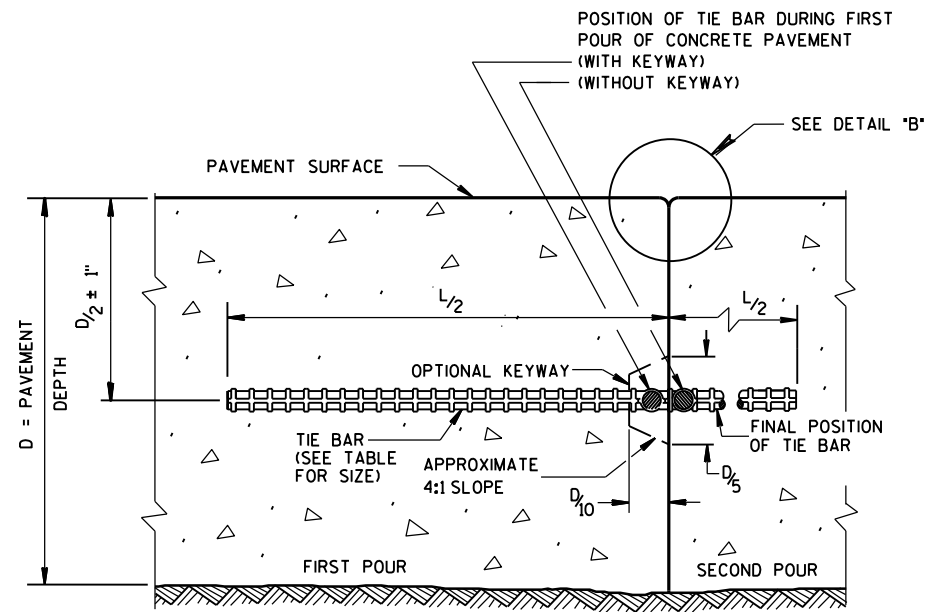
CONCRETE MEDIAN NOSE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 6/8/2006 DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	

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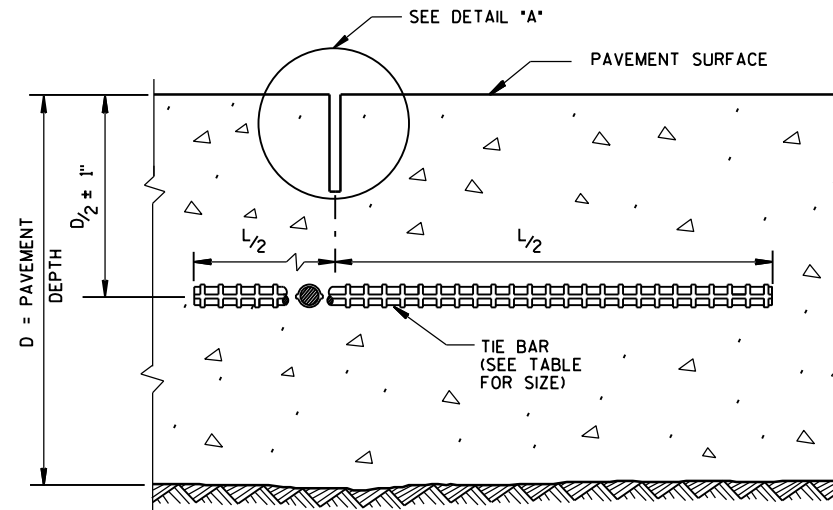
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S.D.D. 11 B 2-2

S.D.D. 11 B 2-2



CONSTRUCTION JOINT



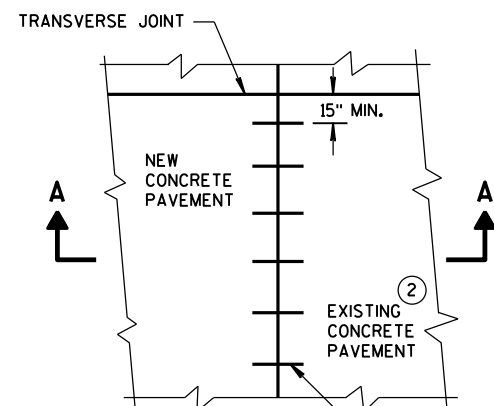
SAWED JOINT

GENERAL NOTES

DO NOT SEAL OR FILL LONGITUDINAL JOINTS.
 CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.

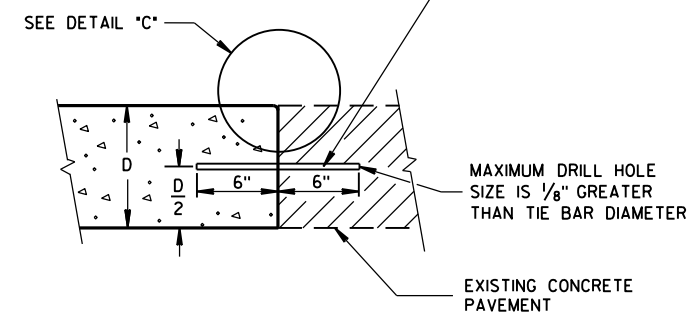
CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

- ① ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
- ② PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

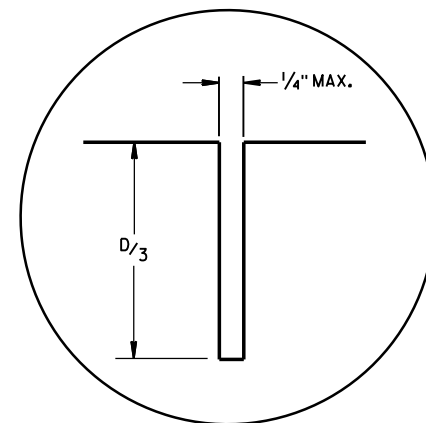


PLAN VIEW

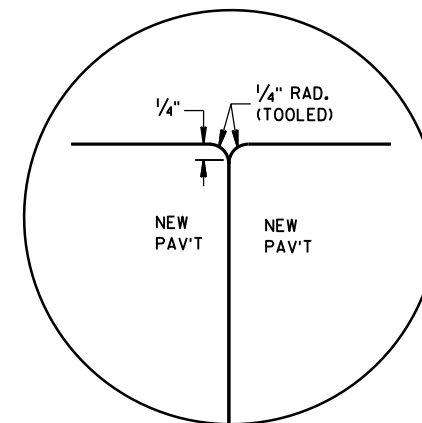
NO. 6 TIE BARS SPACED 30" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT. ①



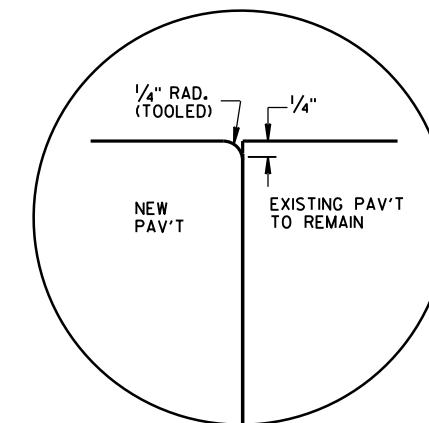
**SECTION A-A
 LONGITUDINAL CONSTRUCTION JOINT
 TIE BARS ANCHORED
 INTO EXISTING PAVEMENT**



DETAIL "A"



DETAIL "B"



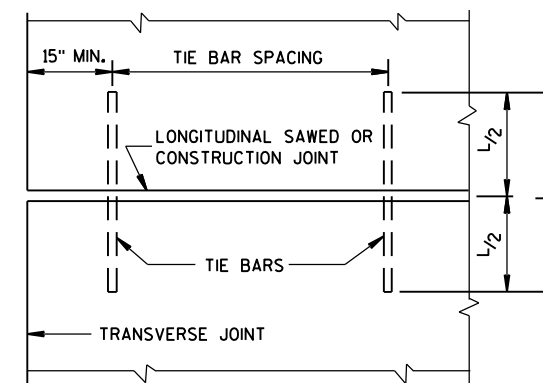
DETAIL "C"

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.



**PLAN VIEW
 SHOWING LOCATION OF TIE BARS**

**CONCRETE PAVEMENT
 LONGITUDINAL JOINTS AND TIES**

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 June, 2015 /S/ Peter Kemp, P.E.
 DATE DATE PAVEMENT SUPERVISOR
 FHWA

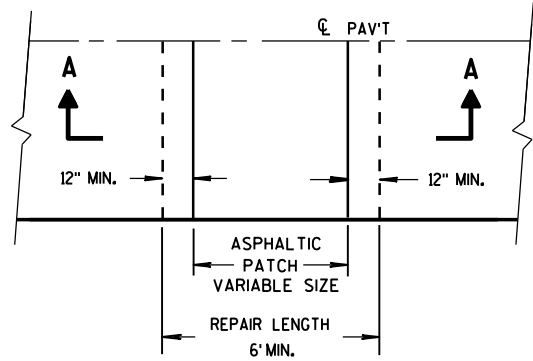
GENERAL NOTES

SAW CUT, DRILL, AND LIFT OUT EXISTING CONCRETE PAVEMENT WITHIN THE BOUNDARIES OF CONCRETE REPAIR AREAS. THE CONTRACTOR MAY MAKE ADDITIONAL SAW CUTS INSIDE THE REPAIR LIMITS TO REDUCE WEIGHT AND SIZE OF CONCRETE PIECES.

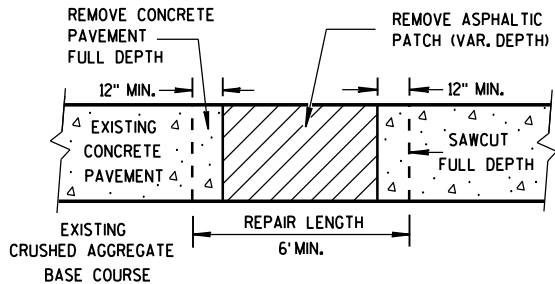
PROVIDE A 6-FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREAS TO ADJACENT TRANSVERSE JOINT OR CRACK IN THE SAME LANE.

THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NONDOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

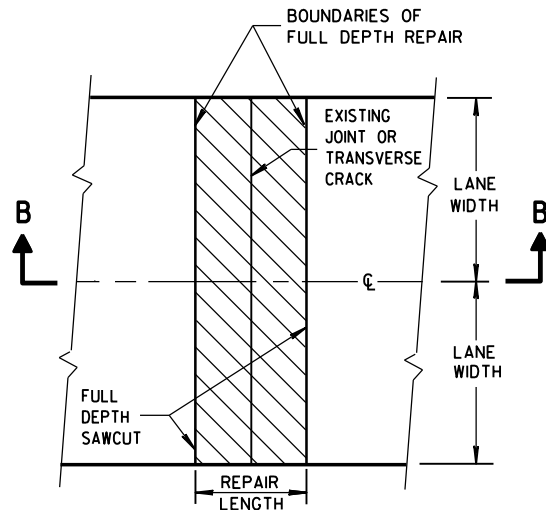
① DOWEL BARS MIGHT NOT EXIST.



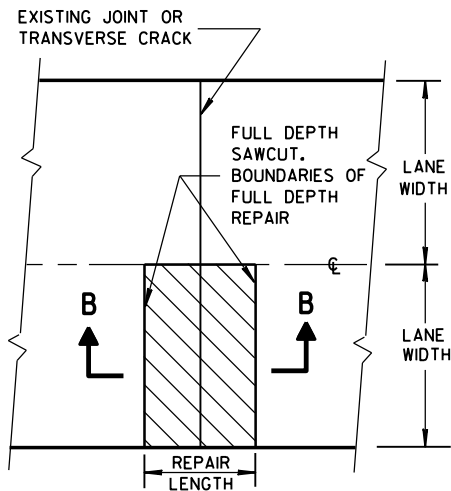
PLAN VIEW



**SECTION A-A
HMA PATCH REMOVAL**

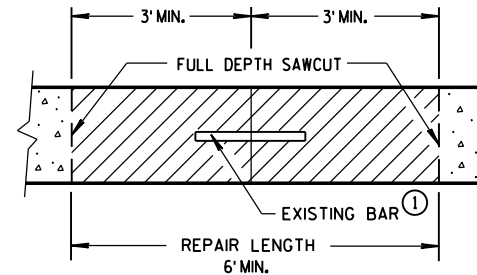


**PLAN VIEW
(DOUBLE LANE REPAIR)**



**PLAN VIEW
(SINGLE LANE REPAIR)**

FULL DEPTH CONCRETE PAVEMENT REMOVAL



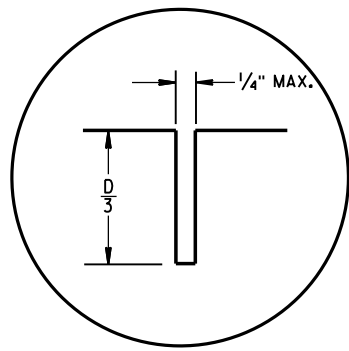
**SECTION B-B
CONCRETE REMOVAL**

6

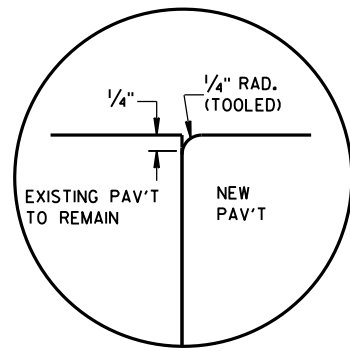
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S.D.D. 13 C 9-14a

S.D.D. 13 C 9-14a

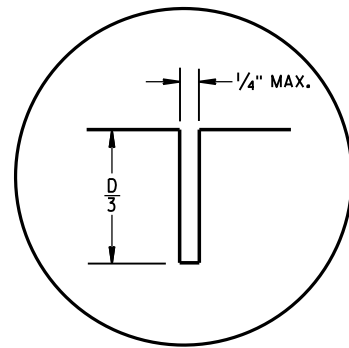


C1

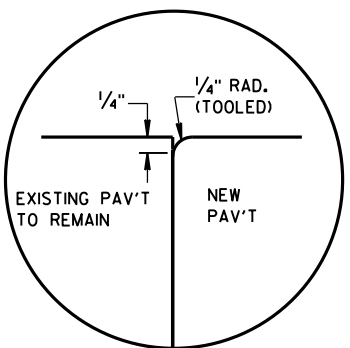


C2

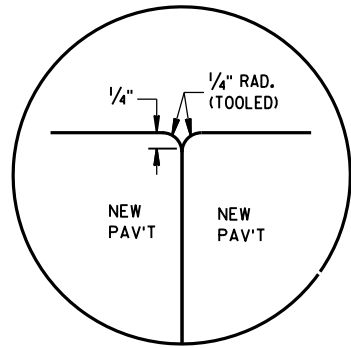
TRANSVERSE JOINTS



L1



L2



L3

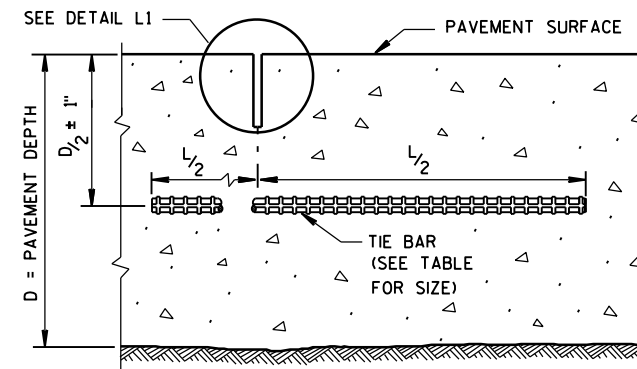
LONGITUDINAL JOINTS

TIE BAR TABLE

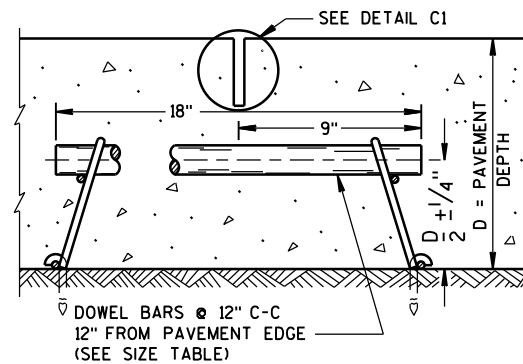
PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4*	30"	24"***

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.



**SECTION C-C
SAWED LONGITUDINAL JOINT**



**SECTION F-F
CONTRACTION JOINT**

GENERAL NOTES

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

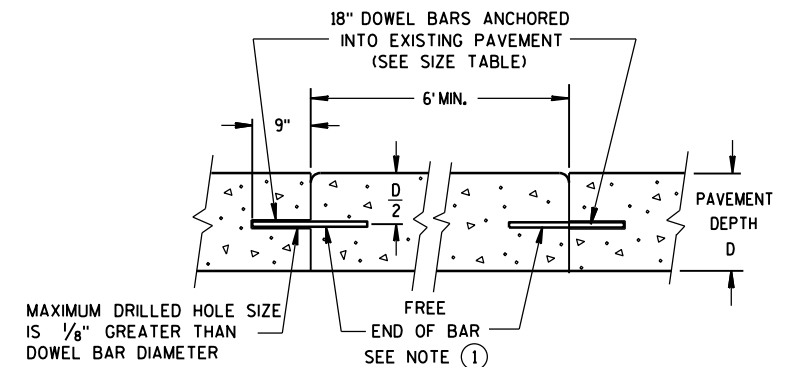
CONCRETE PAVEMENT REPAIRS OF EXISTING NONDOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

DO NOT SEAL OR FILL JOINTS.

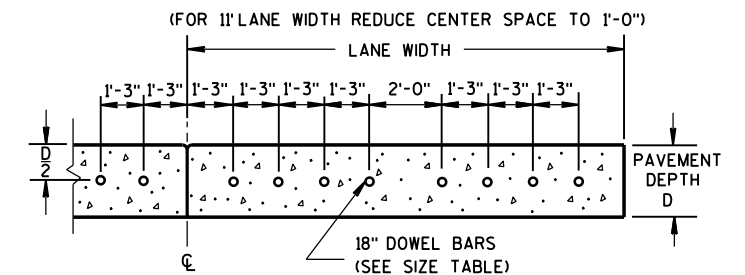
ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

FOR MULTI-LANE CONCRETE PAVEMENT REPLACEMENTS, PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT OR EDGE.

① APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.



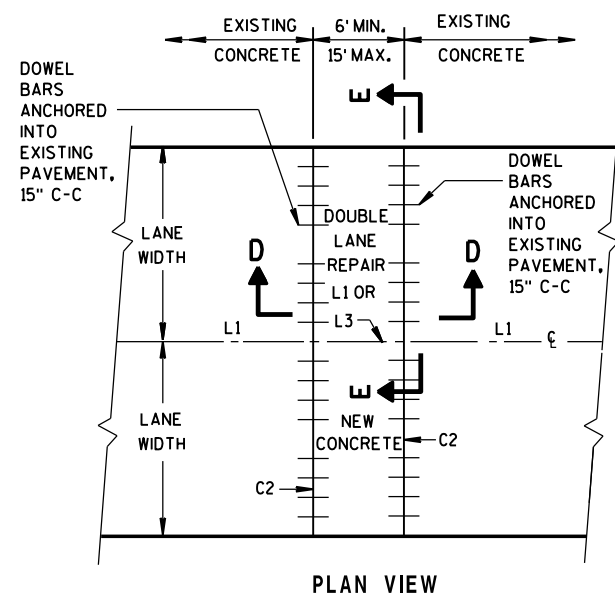
SECTION D-D



**SECTION E-E
DRILLED DOWEL BAR CONSTRUCTION JOINT**

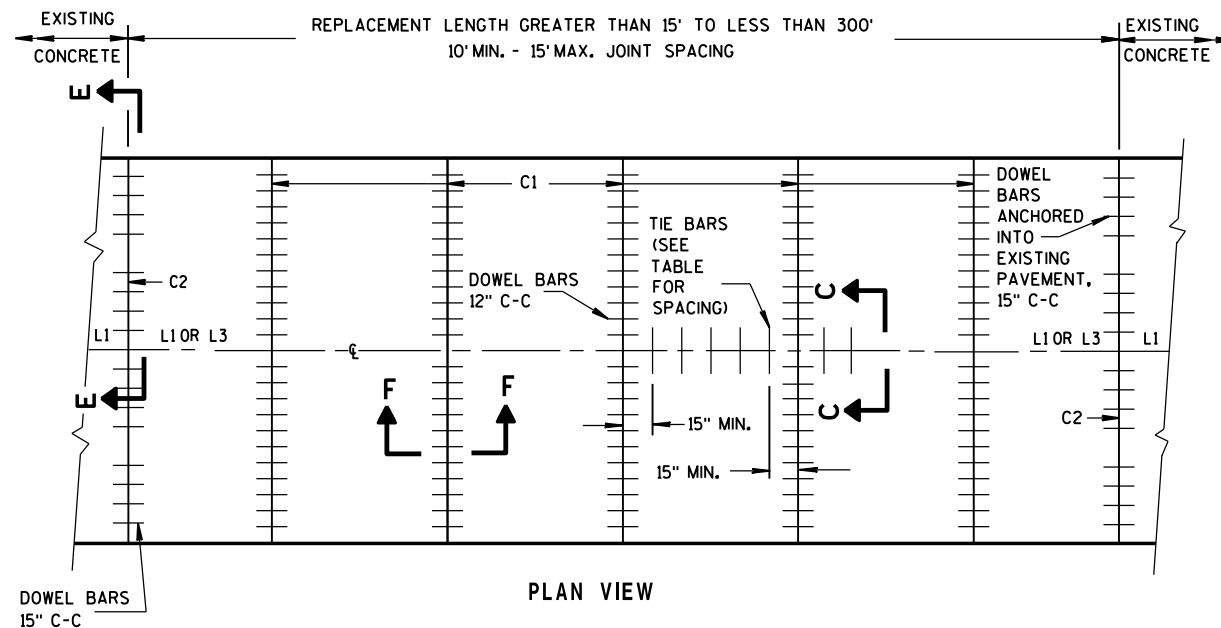
PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	DRILLED DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 1/2", 6", 6 1/2"	NONE	NONE	12'
7", 7 1/2"	1"	1"	14'
8", 8 1/2"	1 1/4"	1 1/4"	15'
9", 9 1/2"	1 1/4"	1 1/4"	15'
10" & ABOVE	1 1/2"	1 1/4"	15'



PLAN VIEW

MULTI-LANE CONCRETE PAVEMENT REPAIR

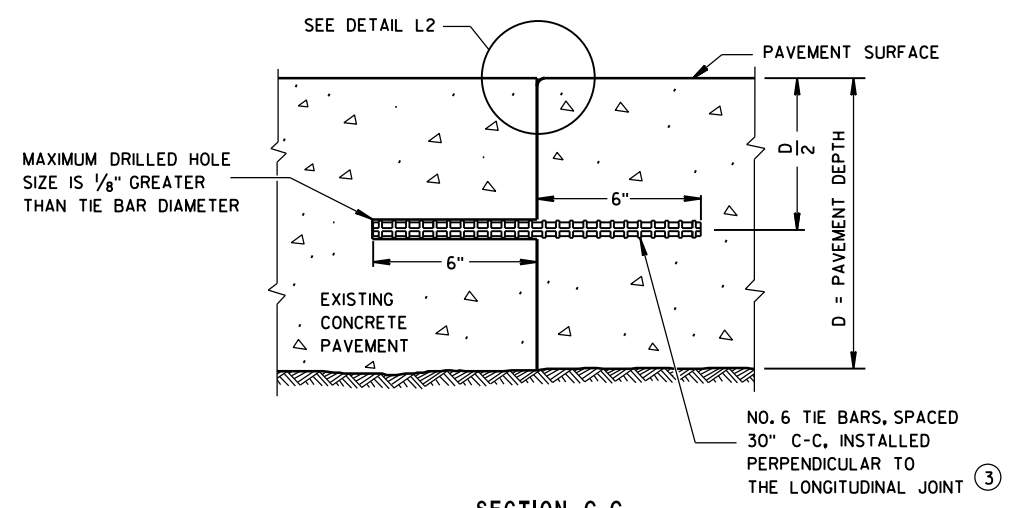


PLAN VIEW

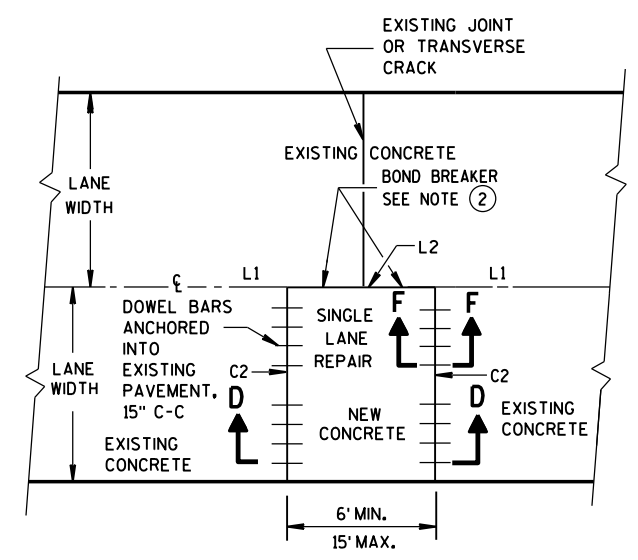
MULTI-LANE CONCRETE PAVEMENT REPLACEMENT

GENERAL NOTES

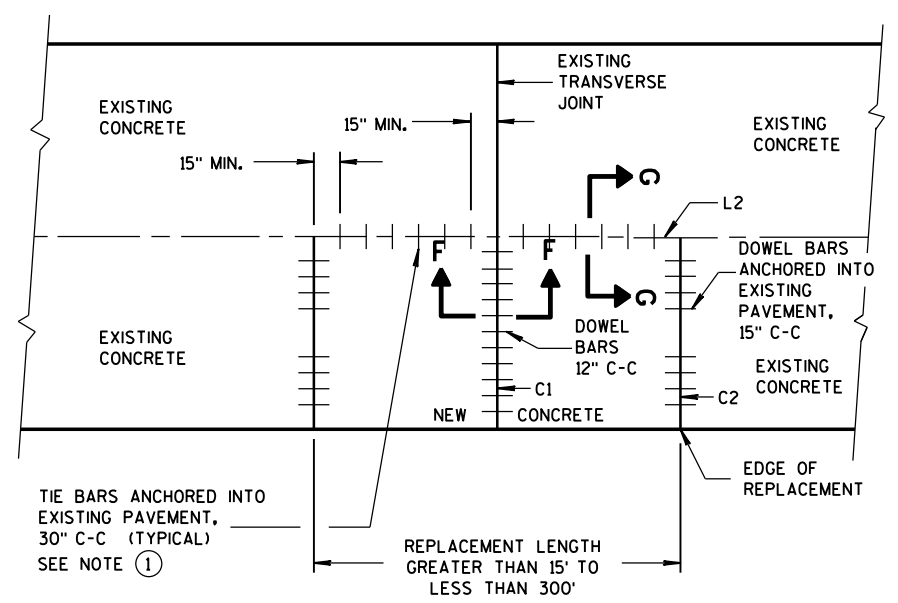
- ① WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, THE CONTRACTOR MAY INSTALL DRILLED TIE BARS ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES IN A HOLE OF SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- ② USE AN ENGINEER-APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.
- ③ ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.



SECTION G-G
**TIE BARS ANCHORED
 INTO EXISTING PAVEMENT**

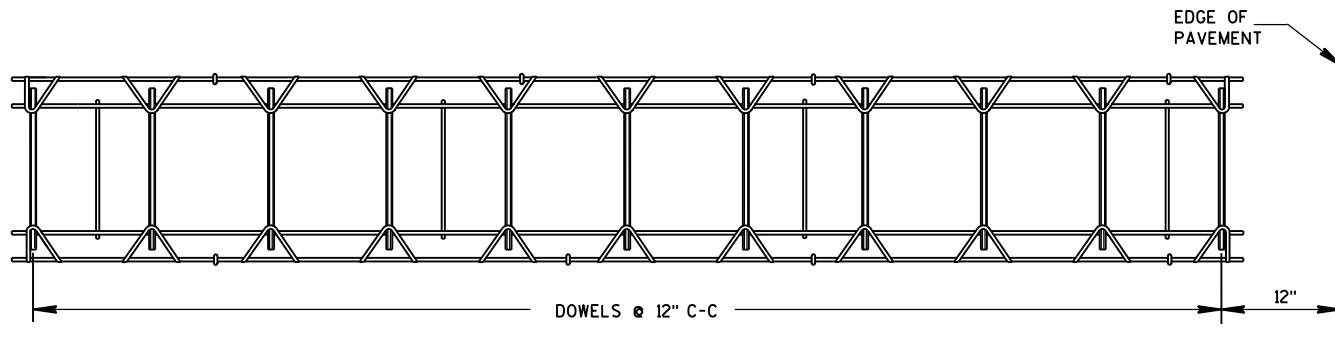


PLAN VIEW
**SINGLE LANE
 CONCRETE PAVEMENT REPAIR**

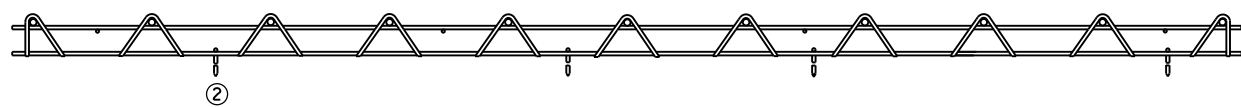


PLAN VIEW
**SINGLE LANE
 CONCRETE PAVEMENT REPLACEMENT**

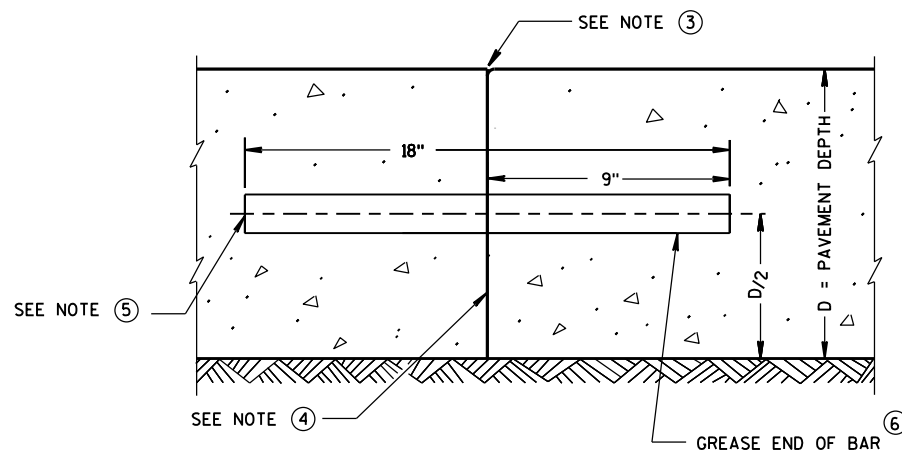
CONCRETE PAVEMENT REPAIR AND REPLACEMENT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March, 2017 DATE	/s/ Peter Kemp, P.E. PAVEMENT SUPERVISOR
FHWA	



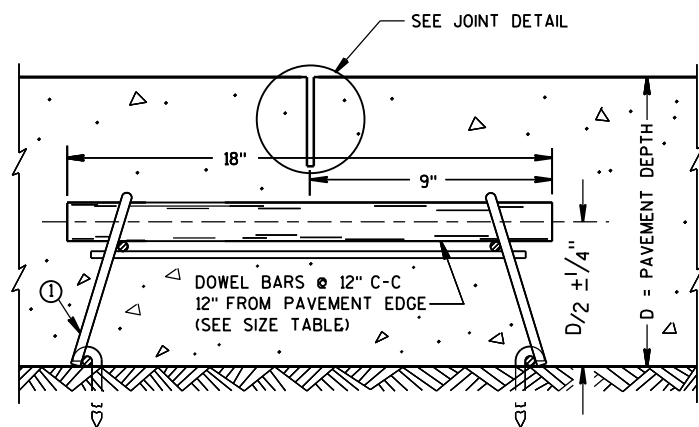
PLAN VIEW



SIDE VIEW
CONTRACTION JOINT DOWEL ASSEMBLY ①



TRANSVERSE CONSTRUCTION JOINT



DOWELED CONTRACTION JOINT

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 1/2", 6", 6 1/2"	NONE	12'
7", 7 1/2"	1"	14'
8", 8 1/2"	1 1/4"	15'
9", 9 1/2"	1 1/4"	15'
10" & ABOVE	1 1/2"	15'

GENERAL NOTES

CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT SEAL OR FILL CONTRACTION JOINTS.

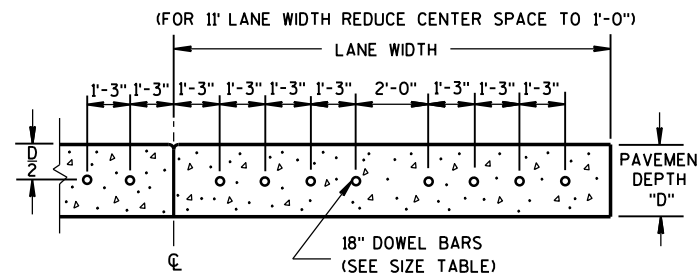
INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE LONGITUDINAL JOINT AND THE FREE EDGE OF PAVEMENT.

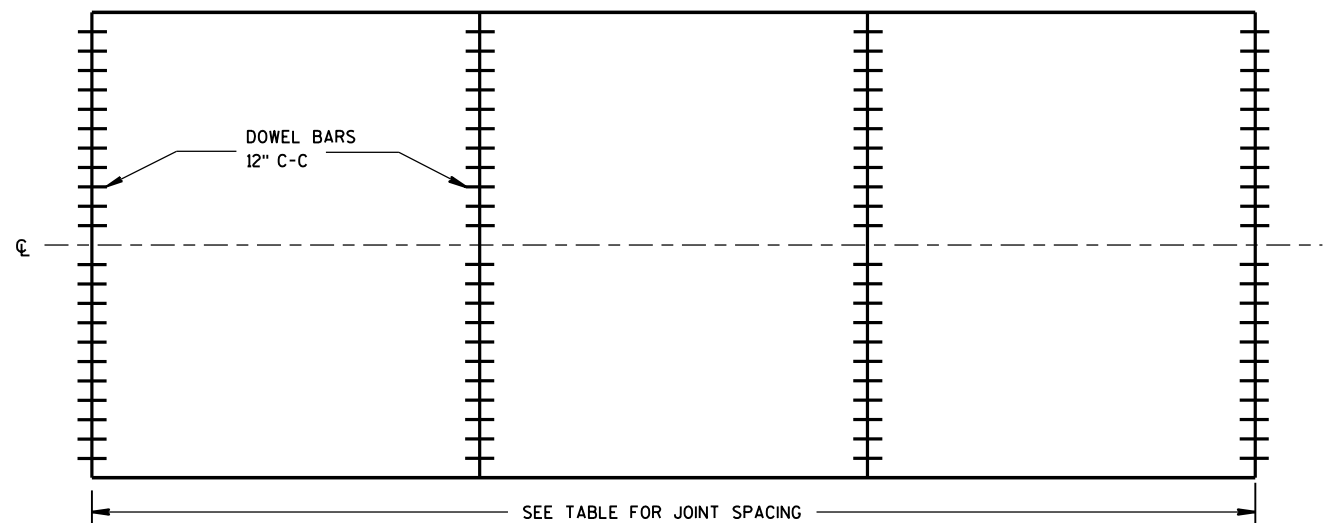
CONSTRUCTION JOINTS

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.

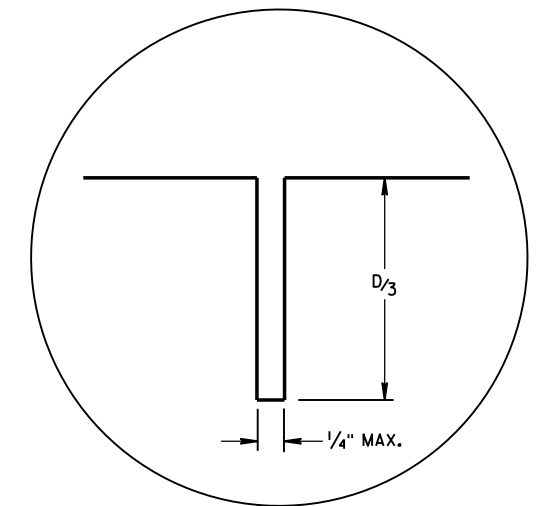
- ① OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.
- ② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- ③ FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/4-INCH RADIUS AT FORMED JOINTS.
- ④ PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- ⑤ INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO *DRILLED DOWEL BAR CONSTRUCTION JOINT* DETAIL.
- ⑥ APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- ⑦ ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS 1/8-INCH GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.



DRILLED DOWEL BAR CONSTRUCTION JOINT ⑦



CONTRACTION JOINT LOCATIONS



JOINT DETAIL

URBAN DOWELED
CONCRETE PAVEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5/3/2013
DATE

/S/ Deb Bischoff
PAVEMENT POLICY & DESIGN ENGINEER

FHWA

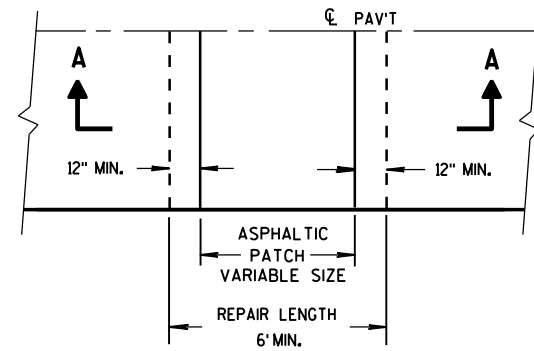
GENERAL NOTES

SAW CUT, DRILL, AND LIFT OUT EXISTING CONCRETE PAVEMENT WITHIN THE BOUNDARIES OF CONCRETE REPAIR AREAS. THE CONTRACTOR MAY MAKE ADDITIONAL SAW CUTS INSIDE THE REPAIR LIMITS TO REDUCE WEIGHT AND SIZE OF CONCRETE PIECES.

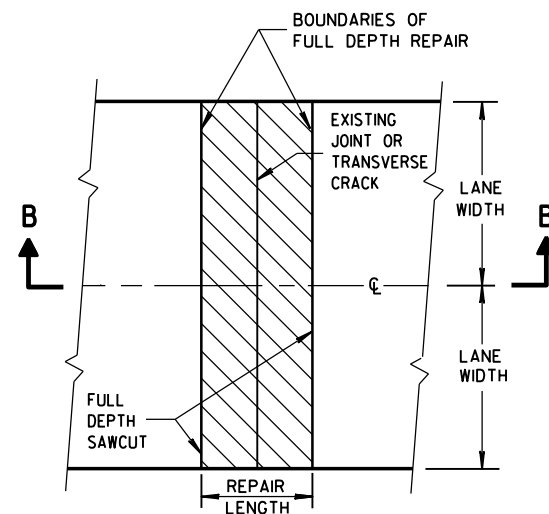
PROVIDE 6-FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREAS TO ADJACENT TRANSVERSE JOINT OR CRACK.

THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NONDOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

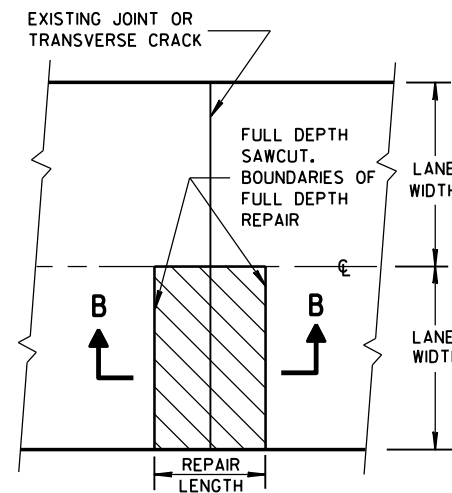
① DOWEL BARS MIGHT NOT EXIST.



PLAN VIEW

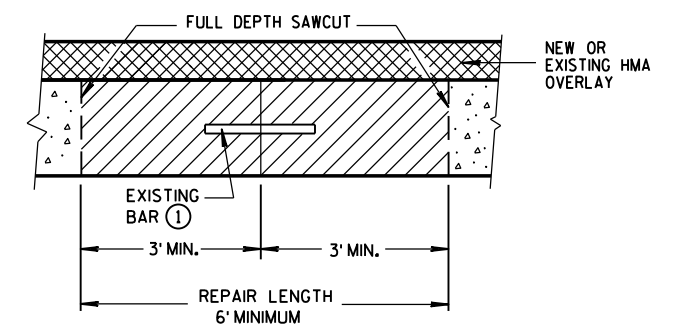


**PLAN VIEW
(DOUBLE LANE REPAIR)**

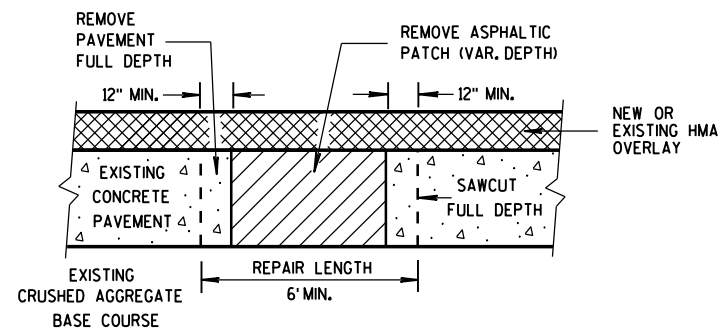


**PLAN VIEW
(SINGLE LANE REPAIR)**

FULL DEPTH CONCRETE PAVEMENT REMOVAL



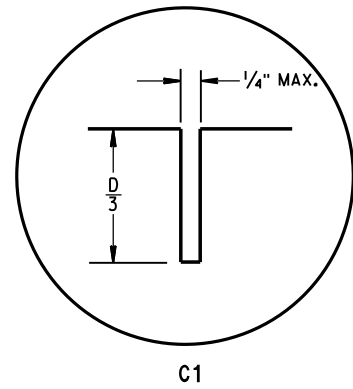
**SECTION B-B
CONCRETE REMOVAL**



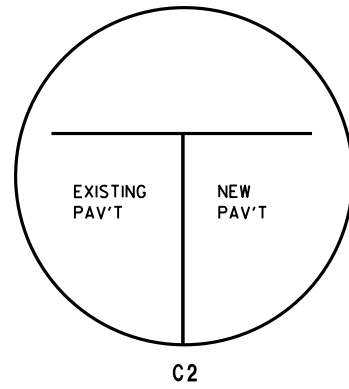
**SECTION A-A
HMA PATCH REMOVAL**

BASE PATCHING CONCRETE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

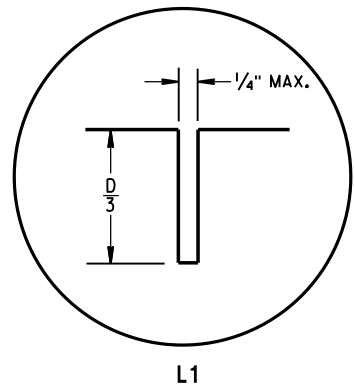


C1

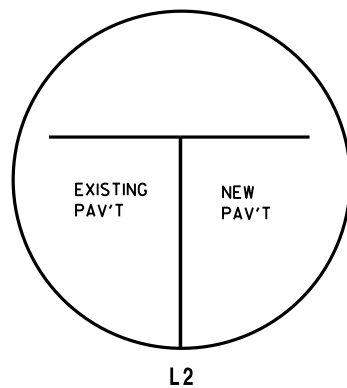


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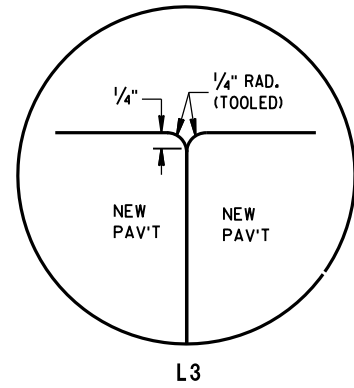
TRANSVERSE JOINTS



L1

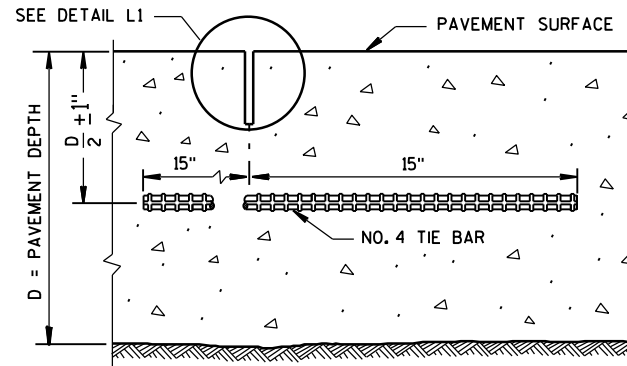


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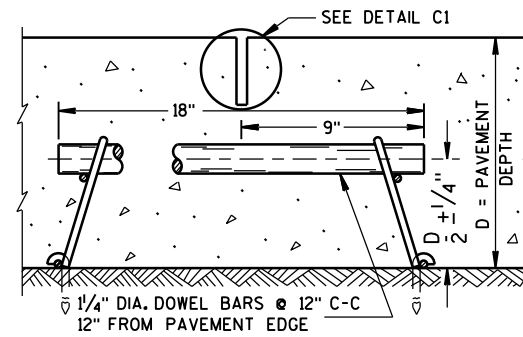


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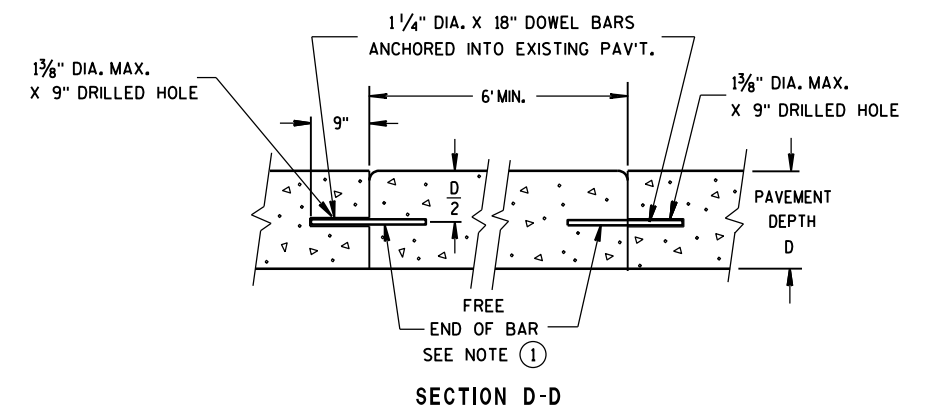
LONGITUDINAL JOINTS



**SECTION C-C
SAWED LONGITUDINAL JOINT**



**SECTION F-F
CONTRACTION JOINT**



SECTION D-D

GENERAL NOTES

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

CONCRETE BASE PATCHES OF EXISTING NONDOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

DO NOT SEAL OR FILL JOINTS.

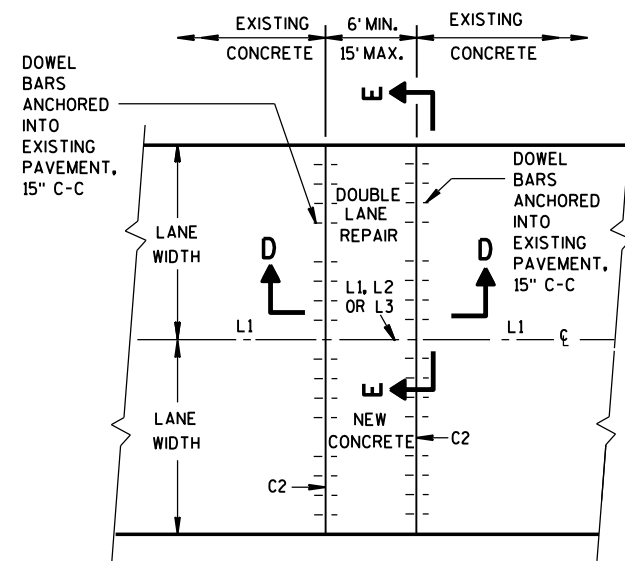
ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM AN EXISTING TRANSVERSE JOINT OR THE EDGE OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT OR EDGE.

① APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.

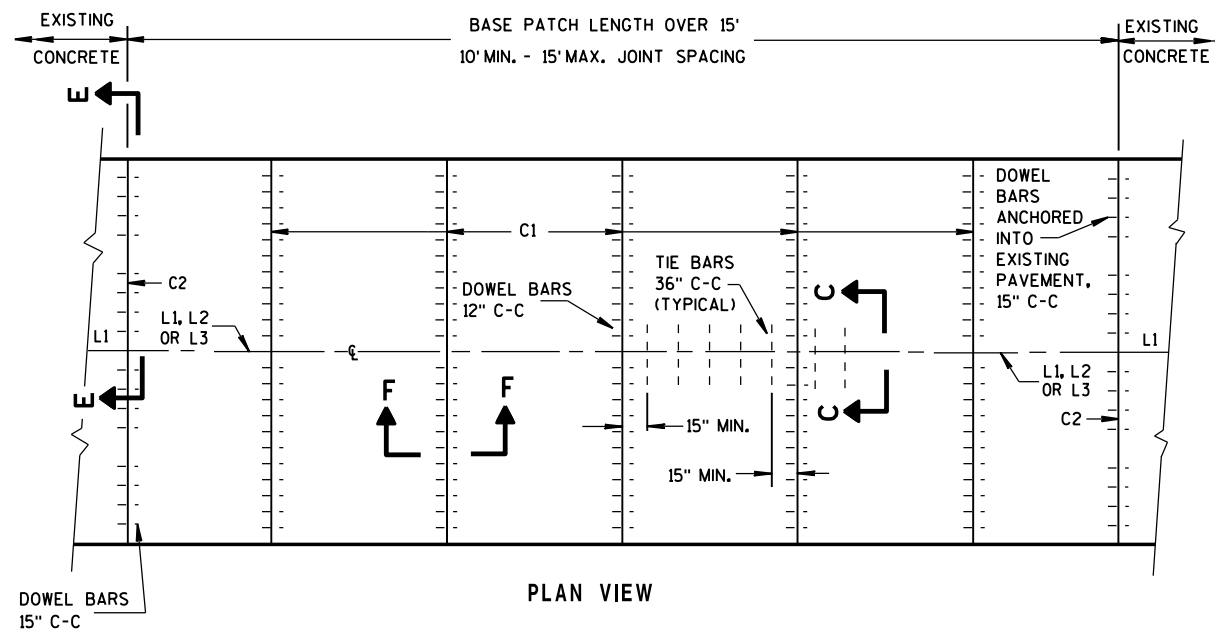
6

6



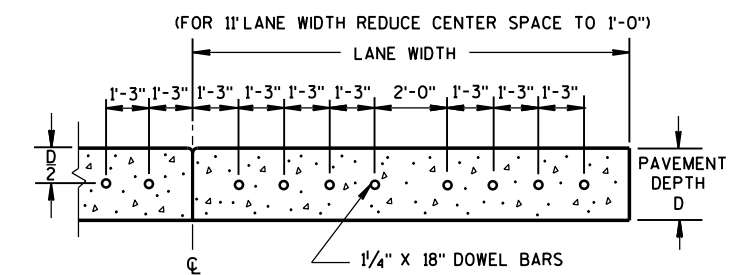
PLAN VIEW

**MULTI-LANE CONCRETE BASE PATCH
15' MAXIMUM LENGTH**



PLAN VIEW

**MULTI-LANE CONCRETE BASE PATCH
GREATER THAN 15' IN LENGTH**



SECTION E-E

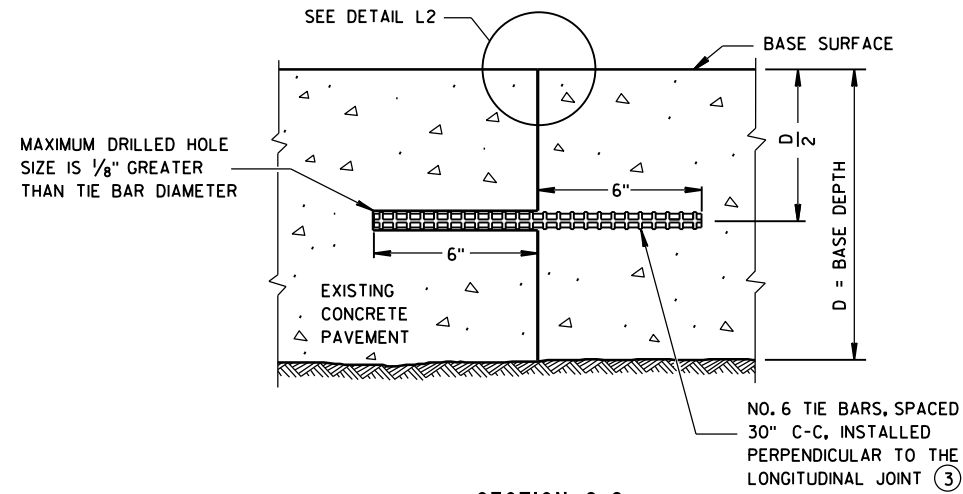
**SPACING OF DOWEL BARS
ANCHORED INTO EXISTING PAVEMENT**

BASE PATCHING CONCRETE

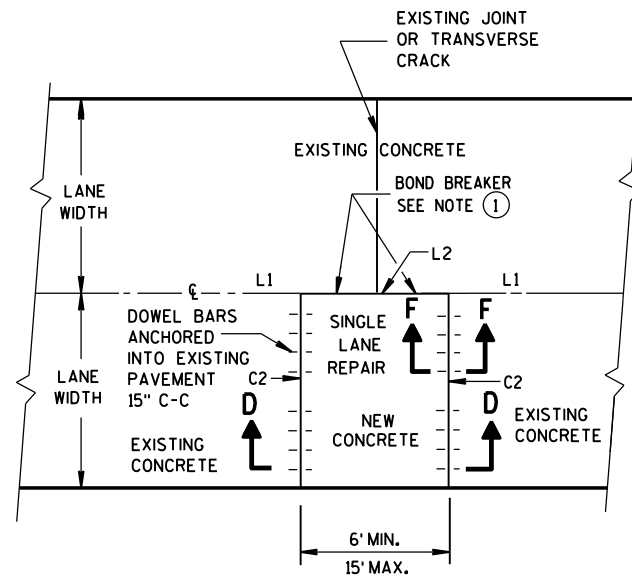
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

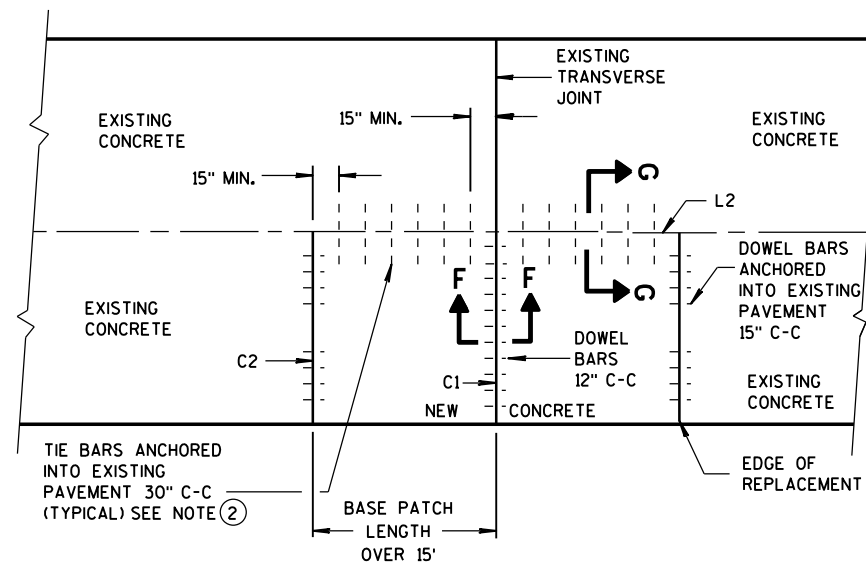
- ① USE AN ENGINEER-APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE BASE PATCHES UP TO 15 FEET IN LENGTH.
- ② WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, DRILLED TIE BARS MAY BE INSTALLED ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES IN A HOLE OF SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- ③ ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.



SECTION G-G
TIE BARS ANCHORED
INTO EXISTING PAVEMENT

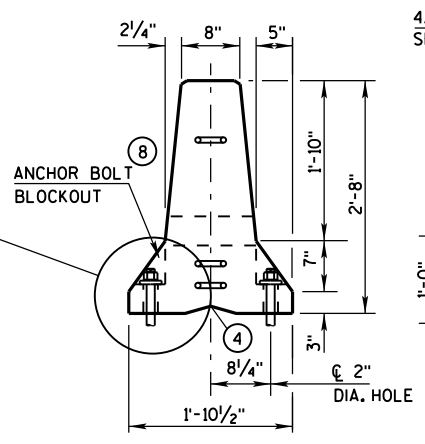
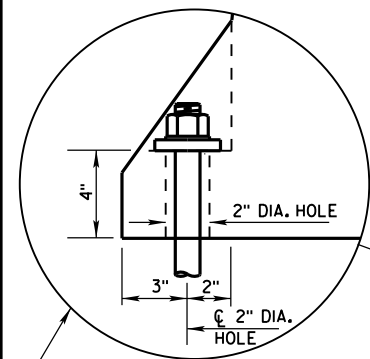


PLAN VIEW
SINGLE LANE CONCRETE BASE PATCH
15' MAXIMUM LENGTH

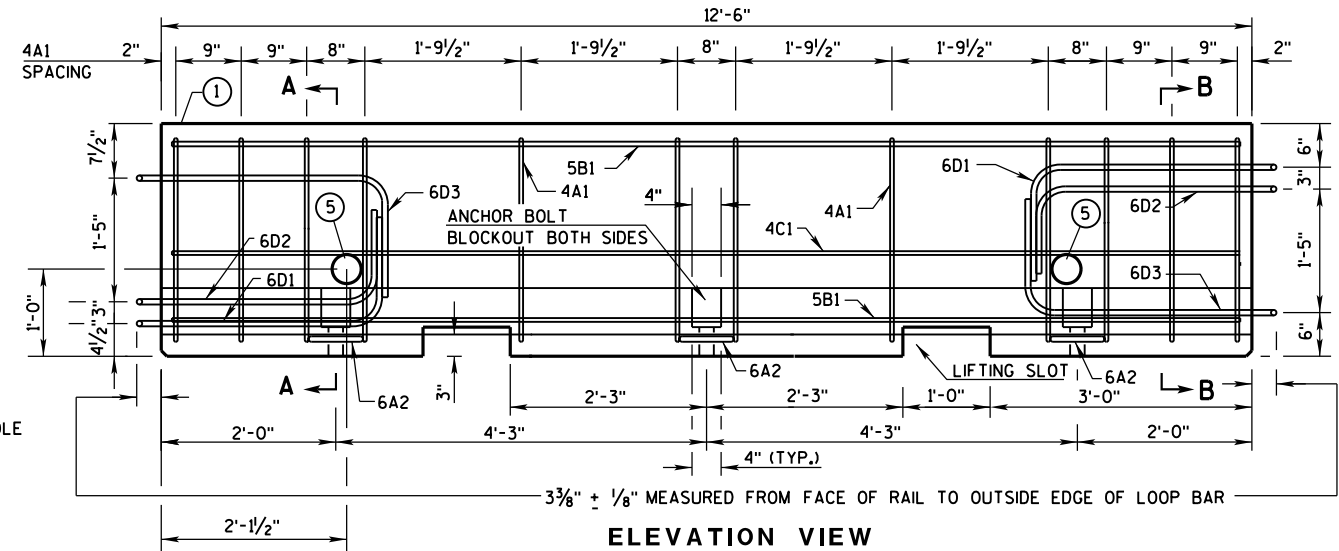


PLAN VIEW
SINGLE LANE CONCRETE BASE PATCH
GREATER THAN 15' IN LENGTH

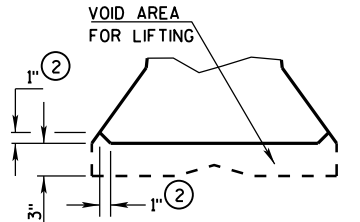
BASE PATCHING CONCRETE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Peter Kemp, P.E. PAVEMENT SUPERVISOR
FHWA	



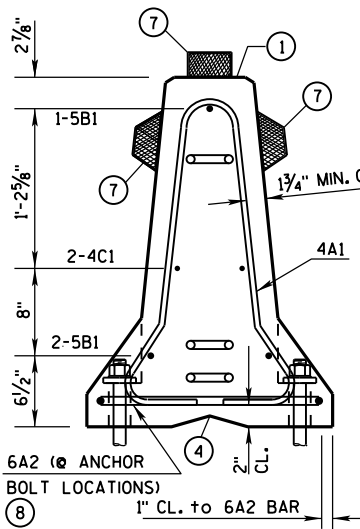
END VIEW



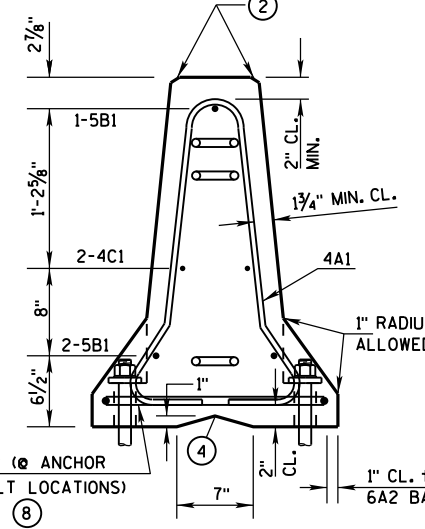
ELEVATION VIEW



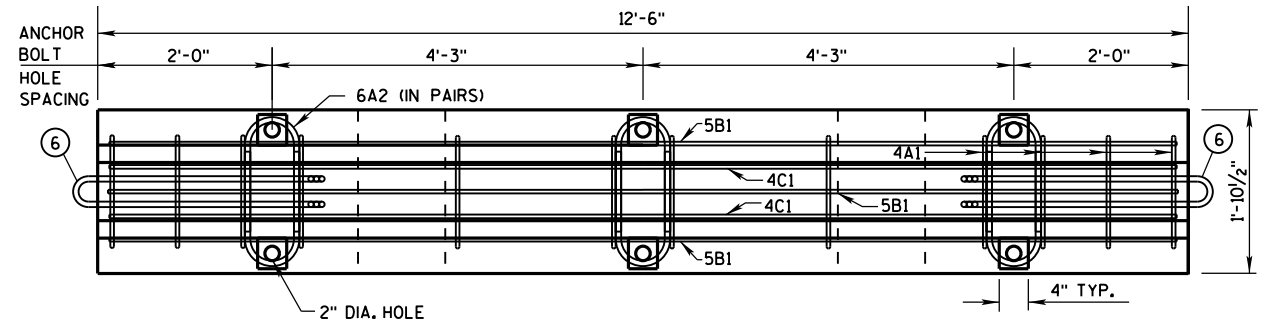
DETAIL "B"
LIFTING SLOT DETAIL



SECTION A-A
(STIRRUP PLACEMENT)



SECTION B-B
(STIRRUP PLACEMENT)



PLAN VIEW

DETAILS OF BARRIER SECTION

GENERAL NOTES

THESE GENERAL NOTES APPLY TO SHEETS 14B7-15(d) THRU 14B7-15(i).

DO NOT INTERMIX CONCRETE BARRIER TEMPORARY PRECAST, 12'-6" (CBTP12.5) WITH OTHER TEMPORARY CONCRETE BARRIERS.

USE ASTM A-615, GRADE 60, DEFORMED STEEL BARS FOR BARS 4A1, 6A2, 5B1 AND 4C1 IN THE BARRIER SECTION AND FOR 4V1, 4V2, 4V3, 4V4, 4V5, 4V6, 4F1, 4F2 AND 5F3 IN THE BARRIER TAPER SECTION.

LOOP BARS 6D1, 6D2 AND 6D3 SHALL BE 3/4" SMOOTH STEEL BARS WITH A MINIMUM YIELD STRENGTH OF 60 KSI, A TENSILE STRENGTH OF NOT LESS THAN 1.25 TIMES THE YIELD STRENGTH BUT A MINIMUM OF 80 KSI, A MINIMUM 14% ELONGATION IN 8 INCHES AND PASSING A 180 DEGREE BEND TEST USING A 3-1/2" PIN BEND DIAMETER FOR BEND TESTS. THE LOOPS SHALL BE INSTALLED WITHIN 1/8" OF THE PLAN DIMENSION.

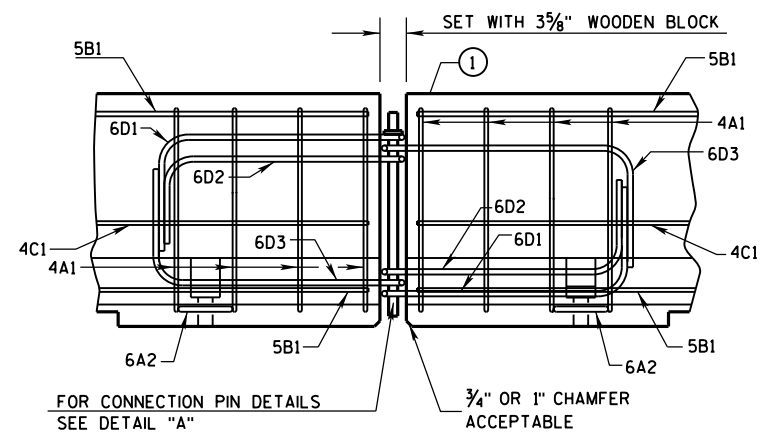
CONSTRUCT LIFTING SLOTS AS SPECIFIED ON THE PLANS TO FACILITATE THE DRAINAGE OF WATER AFTER INSTALLATION.

PLACE BARRIER ON A PAVED SURFACE. REMOVE ALL LOOSE DIRT AND SAND FROM THE ROADWAY SURFACE PRIOR TO PLACEMENT OF THE BARRIER.

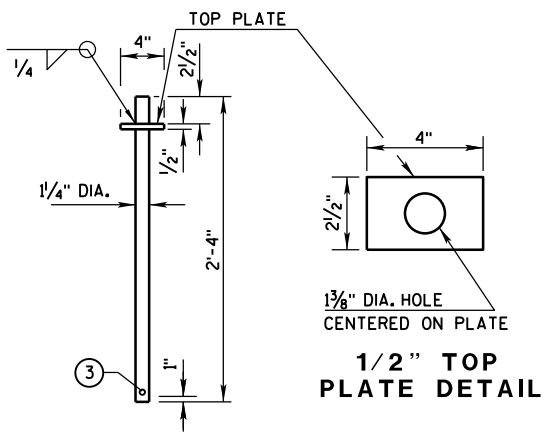
INSTALL MECHANICAL OR ADHESIVE ANCHORS PER MANUFACTURER'S RECOMMENDATIONS. PROVIDE MANUFACTURER'S INFORMATION TO PROJECT ENGINEER.

- ① MARK ONE END OF EACH BARRIER PERMANENTLY BY FORMING INTO THE BARRIER THE FOLLOWING INFORMATION:
 - a. TYPE: WICBTP
 - b. MANUFACTURER
 - c. DATE MANUFACTURED (MONTH AND YEAR)
- ② 1" CHAMFER TO PREVENT SPALLING.
- ③ A 3/8" HOLE IN THE CONNECTION PIN, AT THE LOCATION SHOWN, IS ACCEPTABLE, BUT NOT REQUIRED..
- ④ "V" NOTCH IS OPTIONAL.
- ⑤ THE 4" DIAMETER, 11 GAUGE STEEL, ROUND MECHANICAL TUBING SLEEVE FOR LIFTING (OPTIONAL).
- ⑥ NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.
- ⑦ USE DELINEATORS CONFORMING TO SECTION 633 OF THE STANDARD SPECIFICATIONS. CONTRACTOR MAY USE ALTERNATE SHAPES AND HOUSING. INSTALL DELINEATORS ACCORDING TO MANUFACTURER'S INSTRUCTION. INSTALL YELLOW REFLECTORS WHEN BARRIER IS LOCATED TO THE LEFT OF TRAFFIC AND WHITE REFLECTORS WHEN BARRIER IS LOCATED TO THE RIGHT OF TRAFFIC. SPACE DELINEATORS A MAXIMUM OF 25 FEET APART. PROVIDE TOP MOUNTED DELINEATORS IN ADDITION TO THE SIDE MOUNTED DELINEATORS ON ALL BARRIER INSTALLATIONS LOCATED ON A CURVED ALIGNMENT LONGER THAN 200 FEET AND ON BARRIERS USED TO SEPARATE OPPOSING TRAFFIC.
- ⑧ SEE SHEET D FOR HOW TO ANCHOR BARRIER. SEE SHEET E FOR WHEN TO ANCHOR BARRIER.
- ⑨ 1" CHAMFER OPTIONAL.

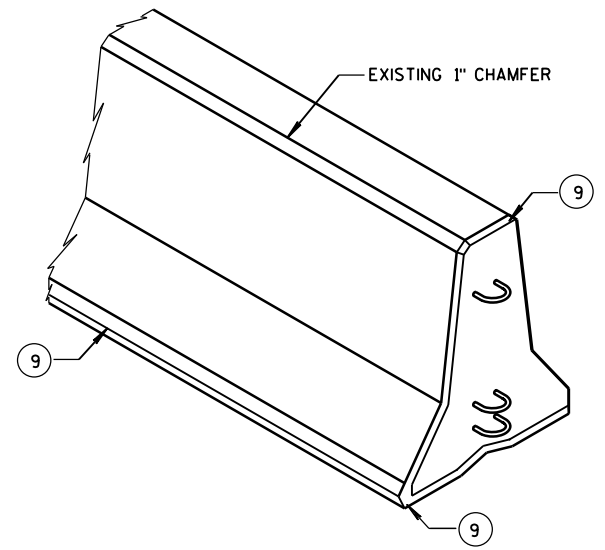
f'c = 4,000 psi



DETAILS OF BARRIER CONNECTION



DETAIL "A"
CONNECTION PIN
(A36 STEEL (10.9 LB EACH))



CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

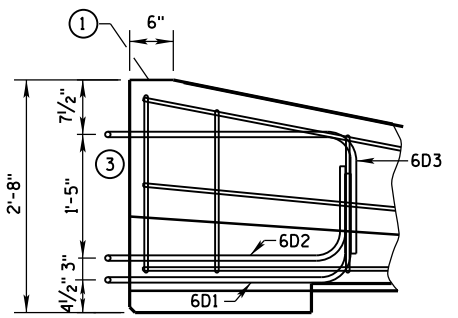
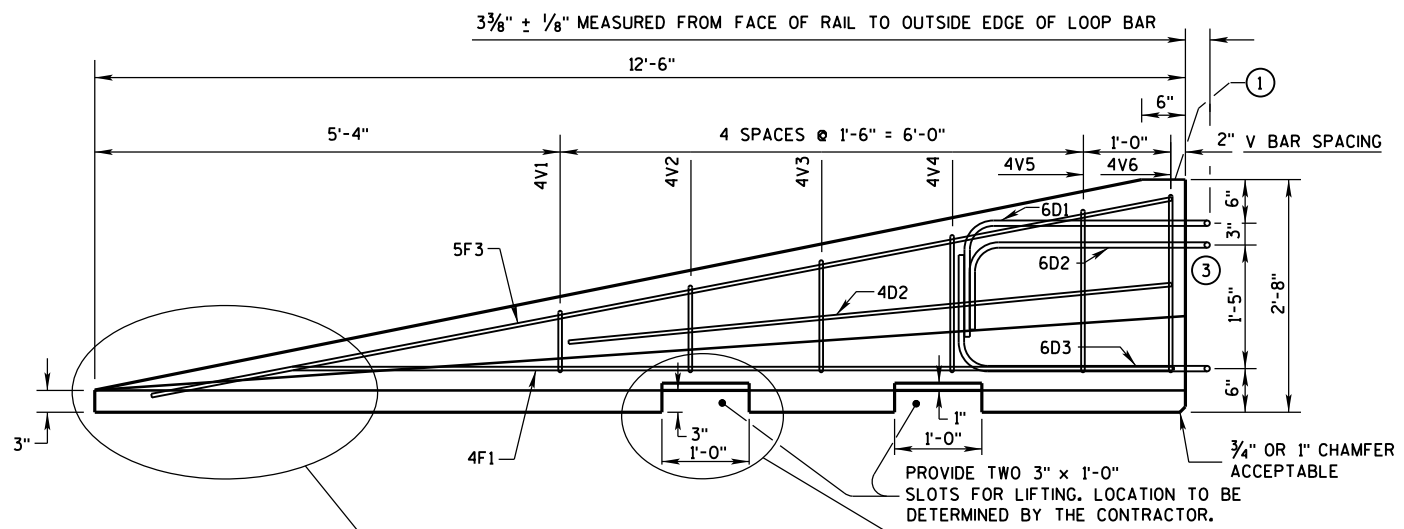
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

S.D.D. 14 B 7-15a

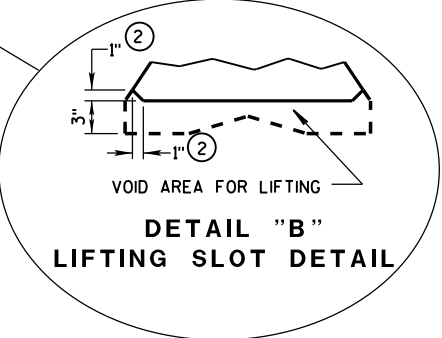
S.D.D. 14 B 7-15a



SIDE ELEVATION
 LOOP BAR ASSEMBLY INVERTED FOR OPPOSITE END.
 (FOR CONNECTION TO RIGHT END OF BARRIER)

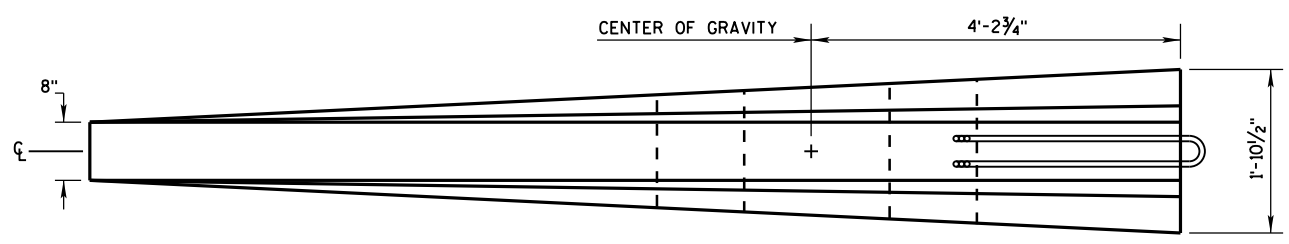
GENERAL NOTES

- ① MARK ONE END OF EACH BARRIER PERMANENTLY BY FORMING INTO THE BARRIER THE FOLLOWING INFORMATION:
 - a. TYPE WICBTP
 - b. MANUFACTURER
 - c. DATE MANUFACTURED (MONTH AND YEAR)
- ② 1" CHAMFER TO PREVENT SPALLING.
- ③ NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.

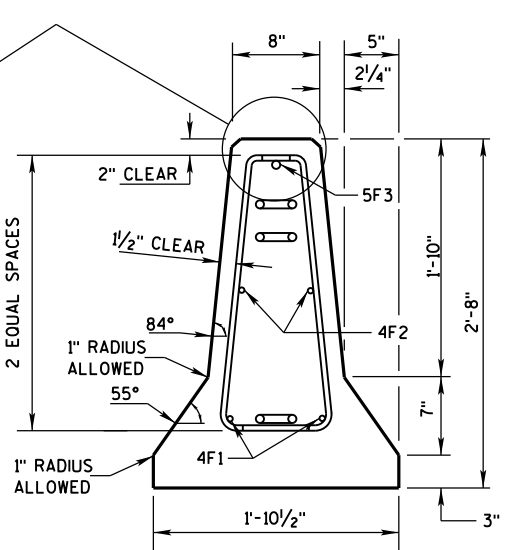
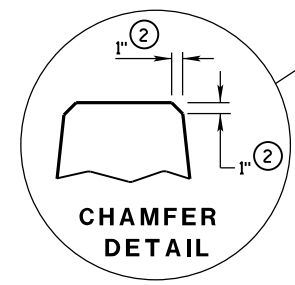


SIDE ELEVATION
 (FOR CONNECTION TO LEFT END OF BARRIER)

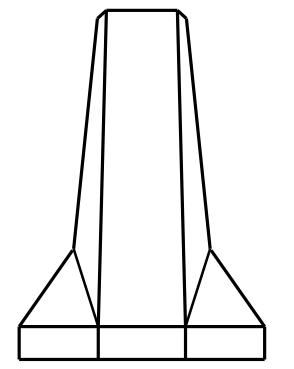
SEE DETAIL "C", BENT BAR DETAIL



PLAN VIEW

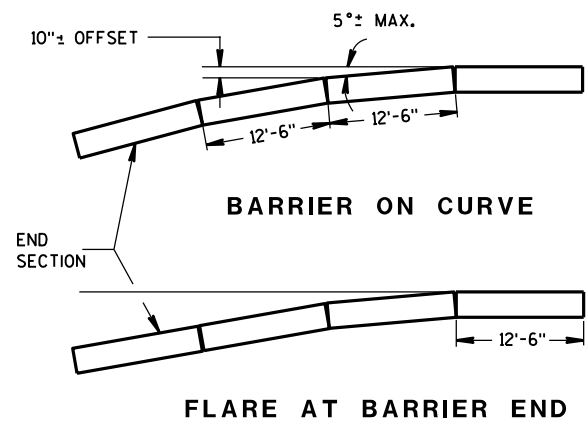


END SECTION



FRONT ELEVATION

DETAILS OF BARRIER TAPER SECTION



POSTED SPEED, (MPH)	FLARE RATE
40 OR LESS	6:1
45 OR GREATER	8:1

**CONCRETE BARRIER
 TEMPORARY PRECAST, 12'-6"**

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

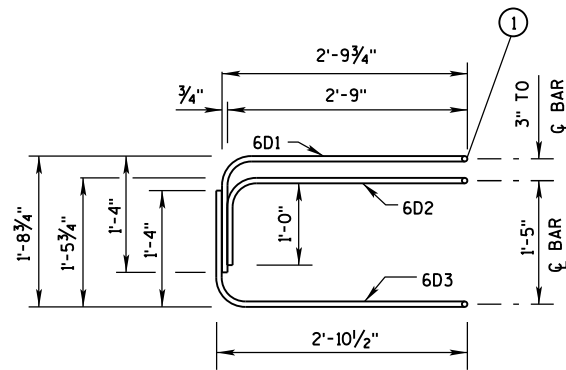
① NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.

**BARRIER TAPER SECTION
BILL OF MATERIALS**

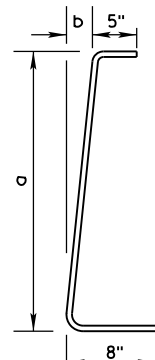
(PER 12'-6" BARRIER TAPER SECTION)

BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
4V1	4	2	1'-11"
4V2	4	2	2'-2"
4V3	4	2	2'-6"
4V4	4	2	2'-9"
4V5	4	2	3'-2"
4V6	4	2	3'-4"
4F1	4	2	12'-0"
4F2	4	2	7'-6"
5F3	5	1	11'-9"

LOOP ASSEMBLY			
BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
6D1	6	1	8'-5"
6D2	6	1	7'-7"
6D3	6	1	8'-6"

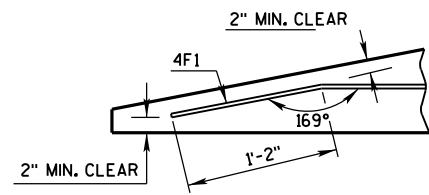


**ELEVATION
LOOP BAR ASSEMBLY**



BAR	a	b
V1	10"	1"
V2	1'-1"	1 1/4"
V3	1'-5"	1 5/8"
V4	1'-8"	1 7/8"
V5	2'-0 1/2"	2 3/8"
V6	2'-3"	2 3/4"

4V BARS
2 AT EACH SIZE REQUIRED
FOR STIRRUP ASSEMBLY



**DETAIL "C"
BENT BAR DETAIL**

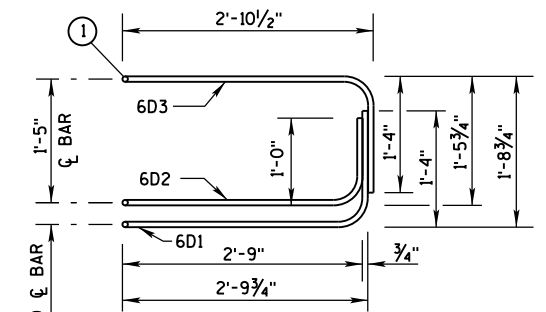
TAPER BARRIER SECTION

**BARRIER SECTION
BILL OF MATERIALS**

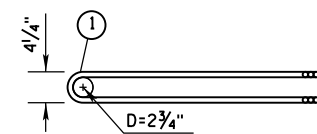
(PER 12'-6" BARRIER SECTION)

BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
4A1	4	12	6'-0"
6A2	6	6	2'-11"
5B1	5	3	12'-2"
4C1	4	2	12'-2"

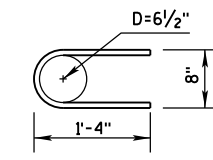
LOOP ASSEMBLY			
BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
6D1	6	2	8'-5"
6D2	6	2	7'-7"
6D3	6	2	8'-6"



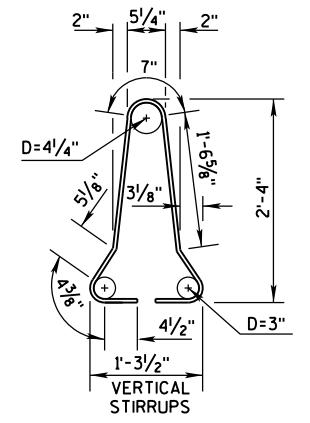
ELEVATION VIEW



**PLAN VIEW
LOOP BAR ASSEMBLY**
(MARKED END SHOWN, INVERT FOR OTHER END)



6A2

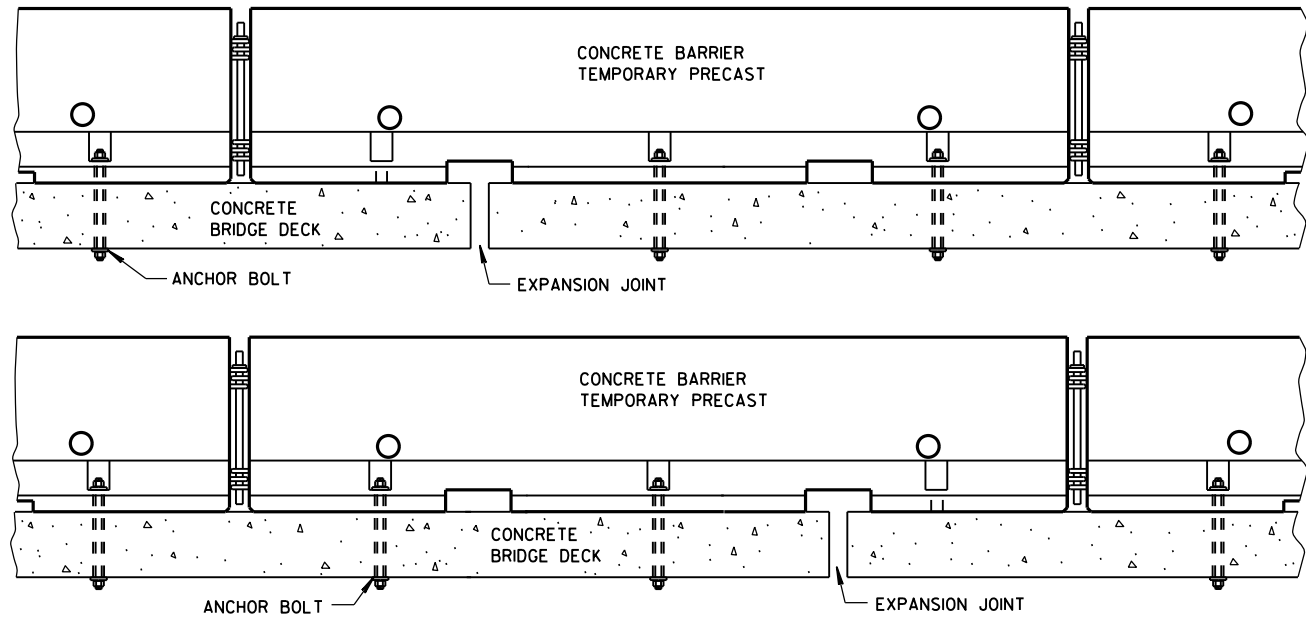


4A1

BARRIER SECTION

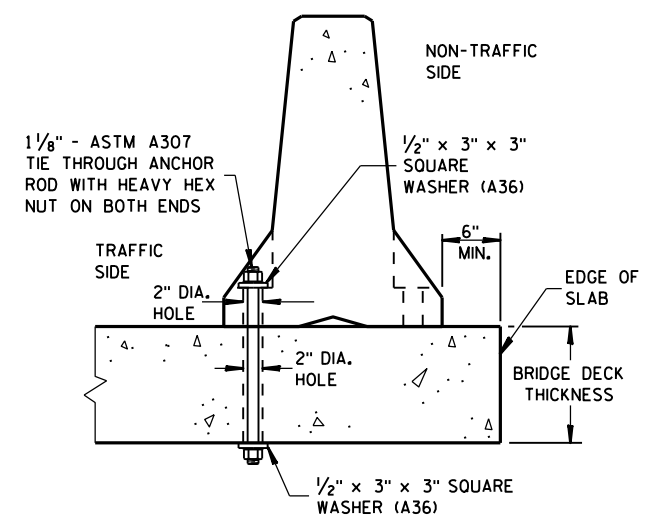
CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



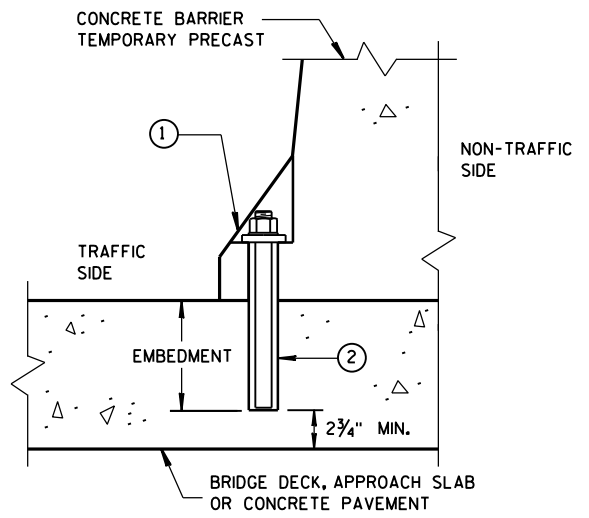
TREATMENT AT BRIDGE DECK EXPANSION JOINTS

(NO SINGLE CONCRETE BARRIER SECTION SHALL BE ANCHORED TO BOTH THE BRIDGE DECK AND THE APPROACH SLAB. ALL ANCHOR BOLT LOCATIONS SHALL BE ANCHORED TO THE DECK IN ACCORDANCE WITH THE DETAIL. NO MORE THAN ONE ANCHOR BOLT SHALL BE ELIMINATED FROM A BARRIER SECTION WHEN SPANNING AN EXPANSION JOINT.)



THROUGH BOLTED ANCHOR INSTALLATION ON BRIDGE DECK

(DO NOT USE ON CONCRETE BRIDGE DECK WITH ASPHALT OVERLAY)



REMOVABLE ADHESIVE ANCHOR INSTALLATION ON CONCRETE BRIDGE DECK, CONCRETE APPROACH SLAB, OR CONCRETE PAVEMENT

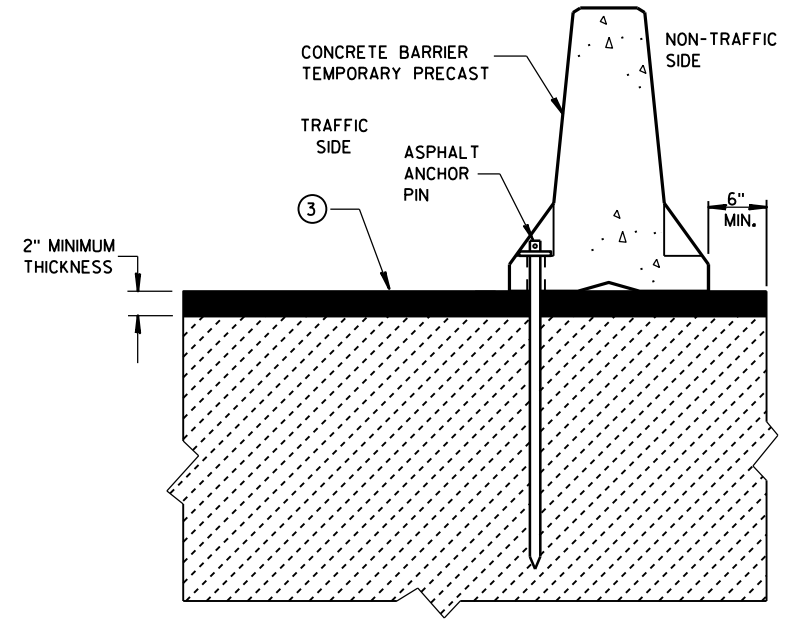
(DO NOT USE ON CONCRETE WITH AN ASPHALTIC OVERLAY)

GENERAL NOTES

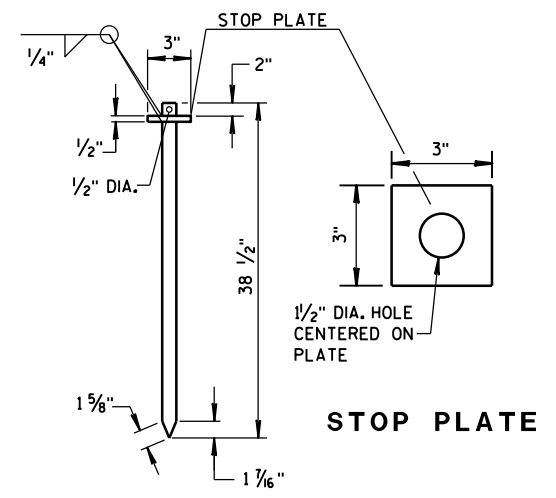
SEE SHEET E FOR WHEN TO ANCHOR. OTHER PARTS OF THE PLAN MAY SHOW ADDITIONAL LOCATIONS REQUIRING ANCHORING.

REMOVE ALL ANCHORS WHEN NO LONGER NEEDED. FILL CONCRETE PAVEMENTS, DECKS AND APPROACH SLABS WITH NON-SHRINK COMMERCIAL GROUT FROM THE APPROVED PRODUCT LIST. FILL ASPHALT PAVEMENTS WITH ASTM D6690 TYPE II RUBBERIZED CRACK FILLER.

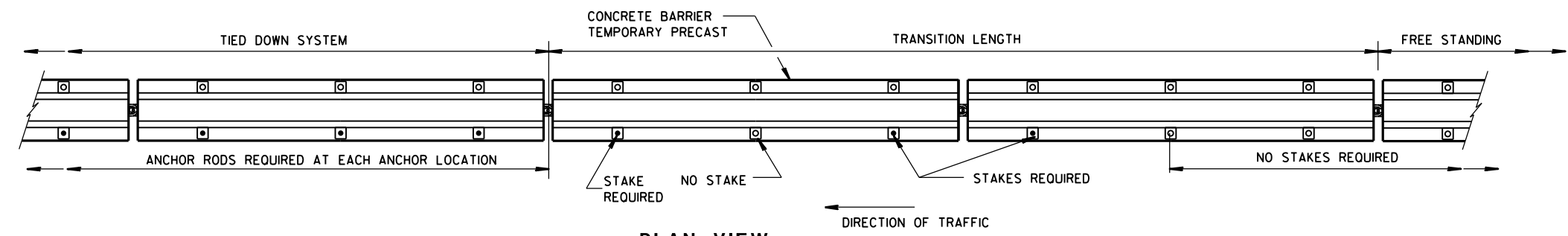
- ① 1/8" DIAMETER A307 THREADED ROD, 1/2" X 3" X 3" SQUARE PLATE WASHER WITH ASTM A36 STEEL, ASTM A563A HEAVY HEX NUT.
- ② ADHESIVE ANCHORS WITH A MINIMUM BOND STRENGTH OF 1,800 PSI AND 5/4" EMBEDMENT. SEE 603.2 AND 603.3.1.2 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.
- ③ ASPHALT SURFACE SHOWN. CONTRACTOR MAY DRILL THROUGH CONCRETE PAVEMENT AND THEN DRIVE ASPHALT ANCHOR PIN.



STAKE DOWN INSTALLATION FOR ASPHALTIC SURFACE



ASPHALT ANCHOR PIN (ASTM A36 STEEL)



FREE STANDING TRANSITION TO TIED-DOWN SYSTEM

(PLACE TRANSITION IN A TANGENT SECTION OF BARRIER PARALLEL TO THE ROADWAY. IF TRANSITION OCCURS ON STRUCTURAL SLAB, ANCHOR AS SHOWN.)

CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

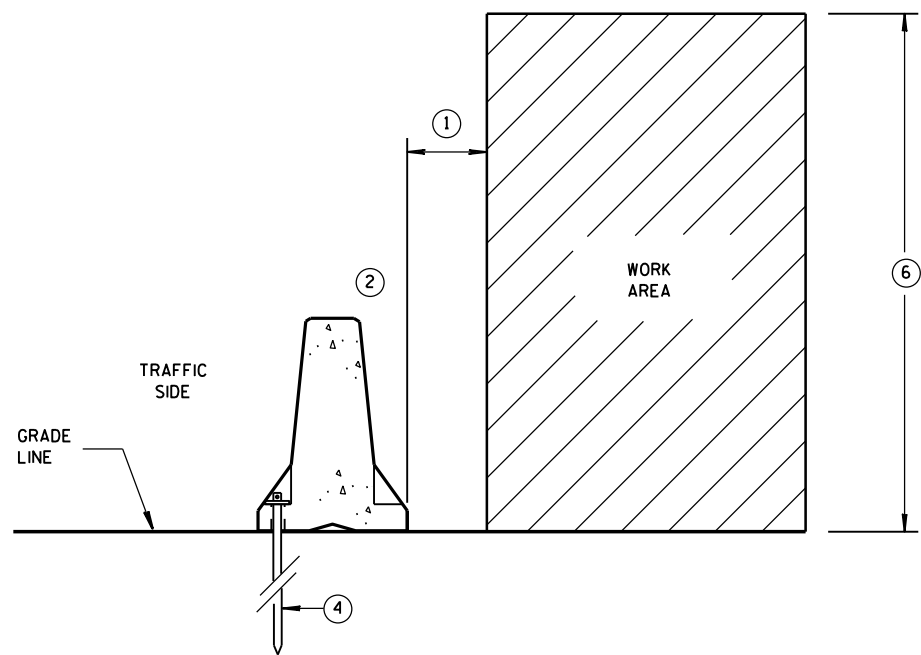
6

S.D.D. 14 B 7-15d

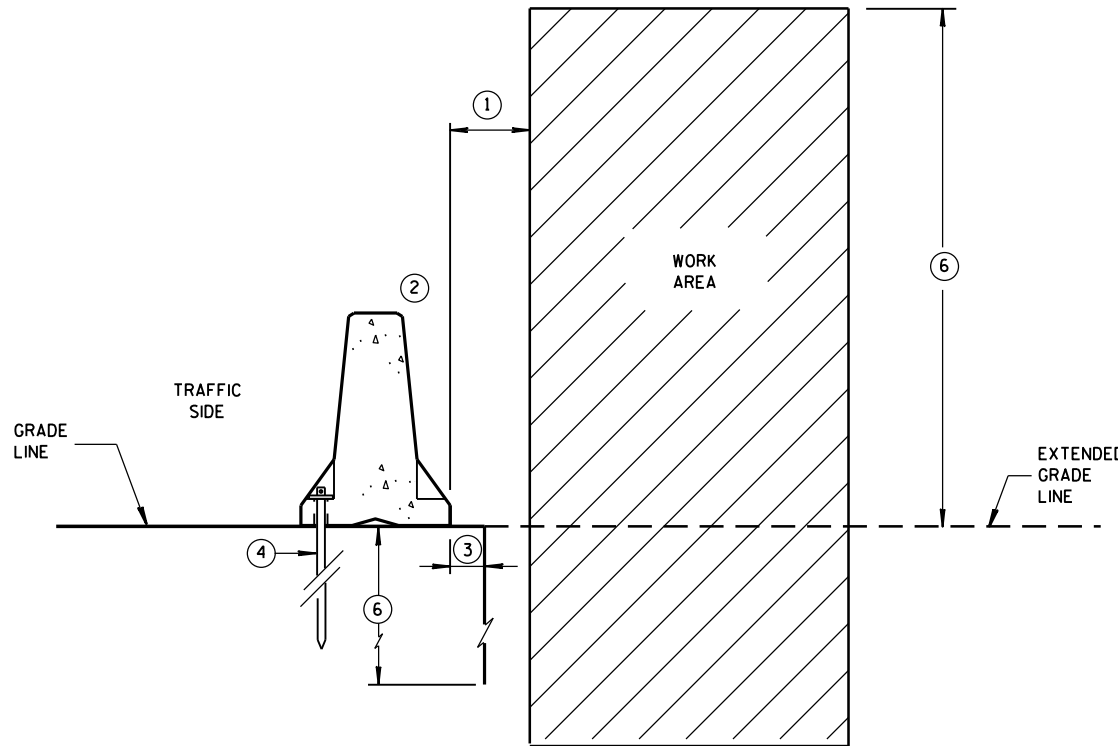
S.D.D. 14 B 7-15d

GENERAL NOTES

- ① WHEN OBJECTS EXTEND ABOVE THE GRADE, A MINIMUM OF 1 FOOT IS REQUIRED FROM BACK OF BARRIER TO OBJECT. SEE OTHER DETAILS FOR FOR THE MINIMUM OFFSET FROM BACK OF BARRIER TO SLOPES OR VERTICAL DROPS.
- ② OBJECTS ARE NOT TO BE PLACED ON, MOUNTED TO, OR LEANED AGAINST THE BARRIER WITHOUT PERMISSION OF THE PROJECT ENGINEER.
- ③ SEE OTHER DETAIL ON SHEET "D" FOR SPACE REQUIREMENTS.
- ④ SEE BOLT THROUGH DECK, REMOVABLE ADHESIVE ANCHOR, OR A STAKE DOWN FOR ASPHALTIC SURFACE TREATMENT DETAILS. ASPHALTIC ANCHOR SHOWN.
- ⑤ DEPTH OF 3 FEET OR MORE.
- ⑥ Y = 6'-6".

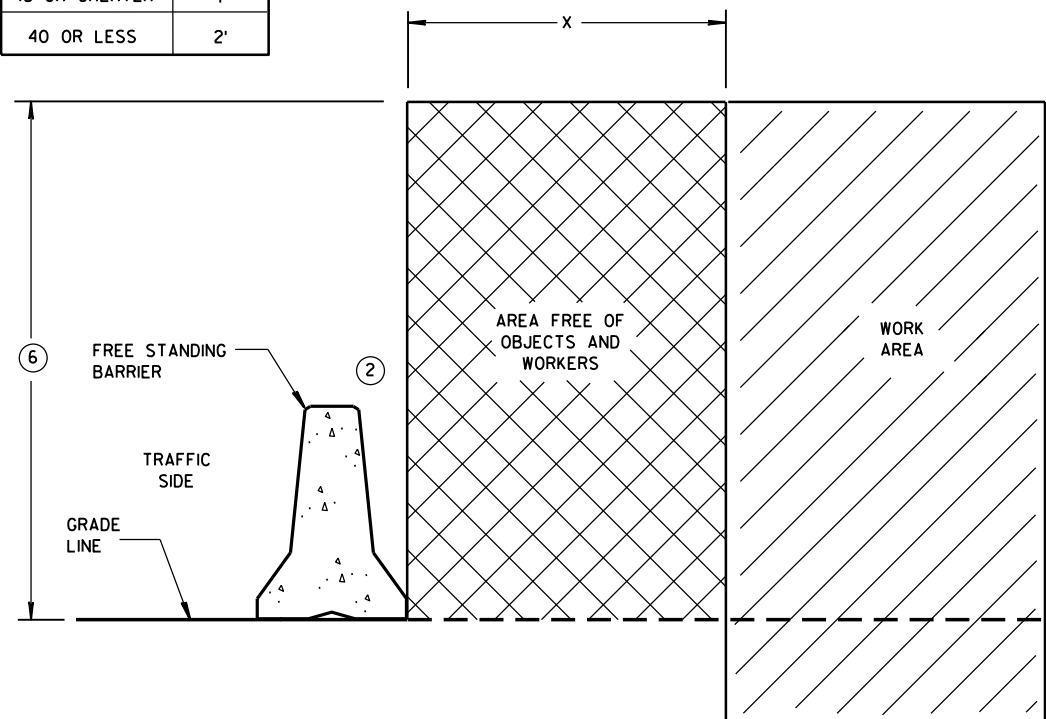


ANCHORED BARRIER SPACE REQUIREMENTS FOR HAZARDS EXTENDED ABOVE THE GRADE LINE

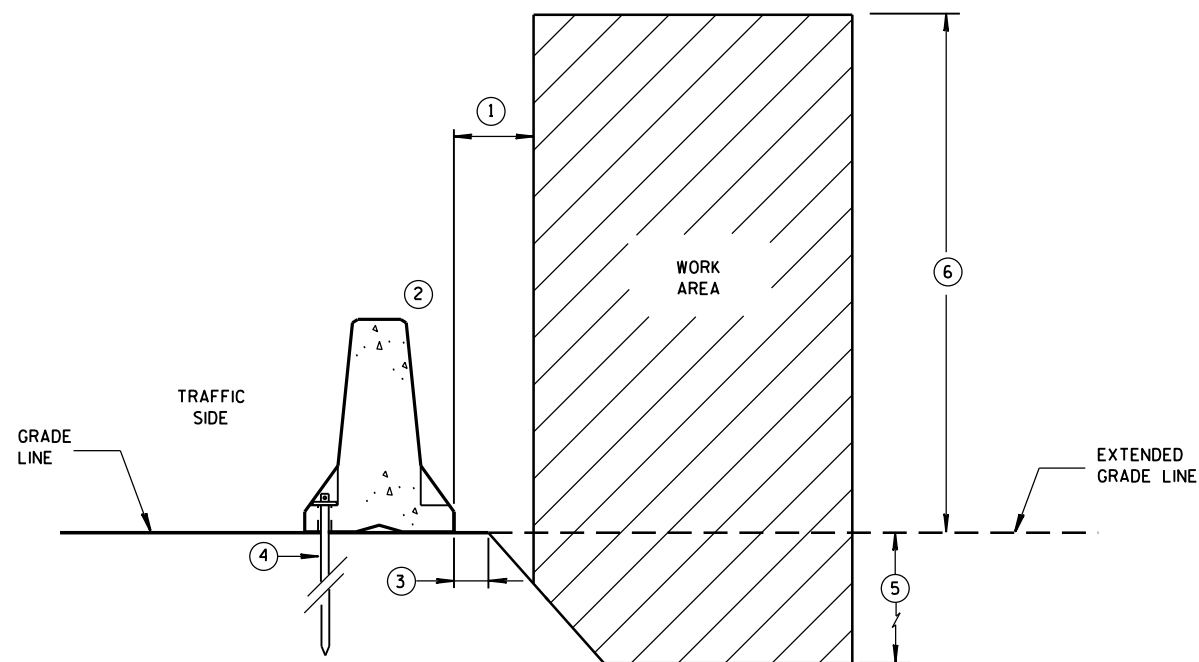


ANCHORED BARRIER SPACE REQUIREMENTS ON VERTICAL DROP OFFS

POSTED SPEED MPH	X
45 OR GREATER	4'
40 OR LESS	2'



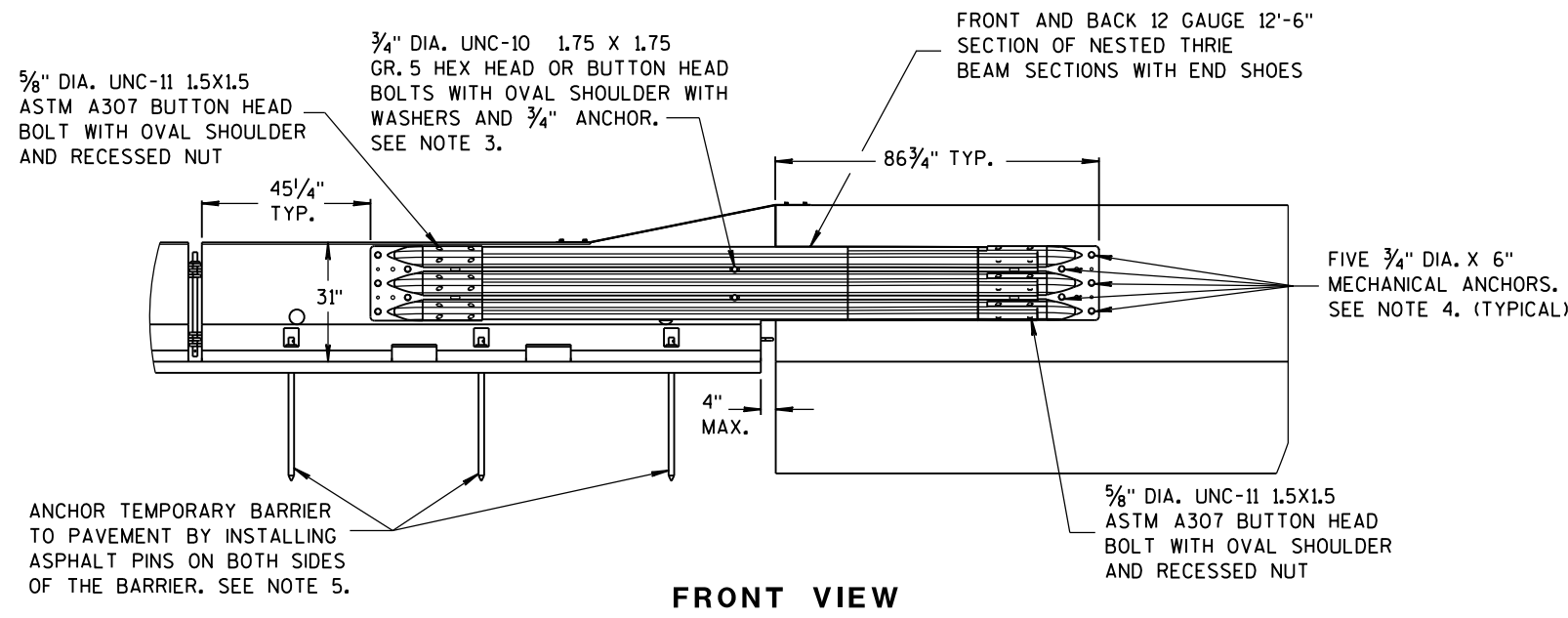
FREE STANDING BARRIER SPACE REQUIREMENTS



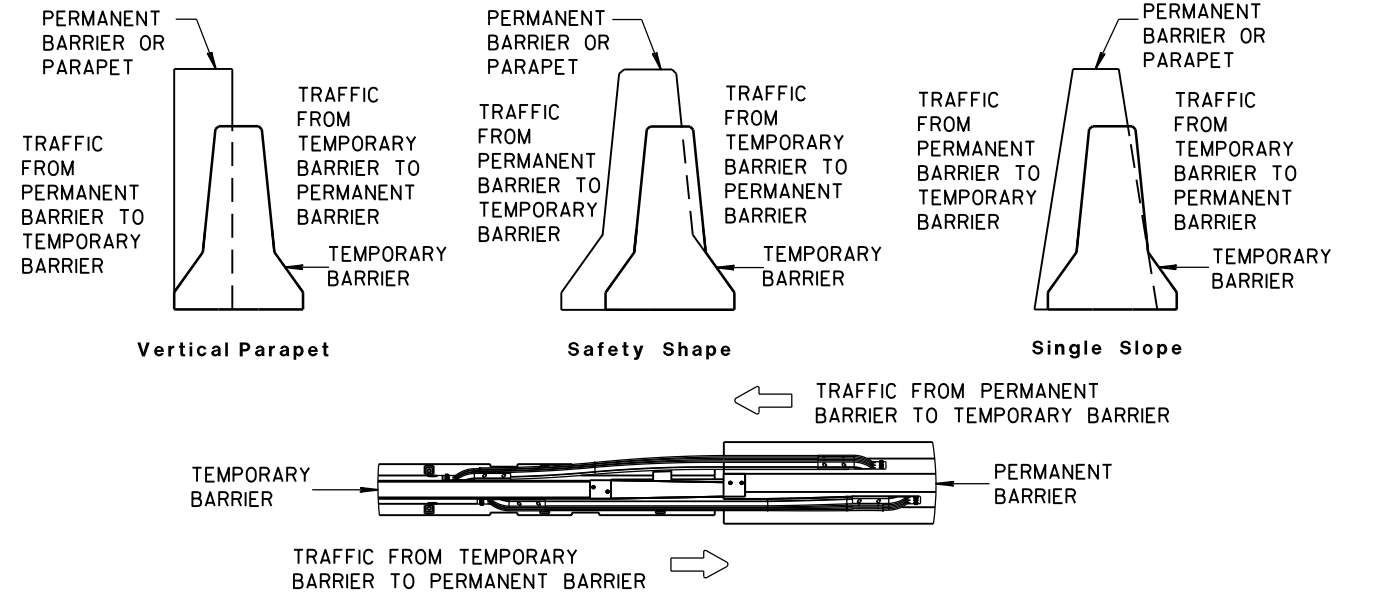
ANCHORED BARRIER SPACE REQUIREMENTS ON SLOPES

**CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

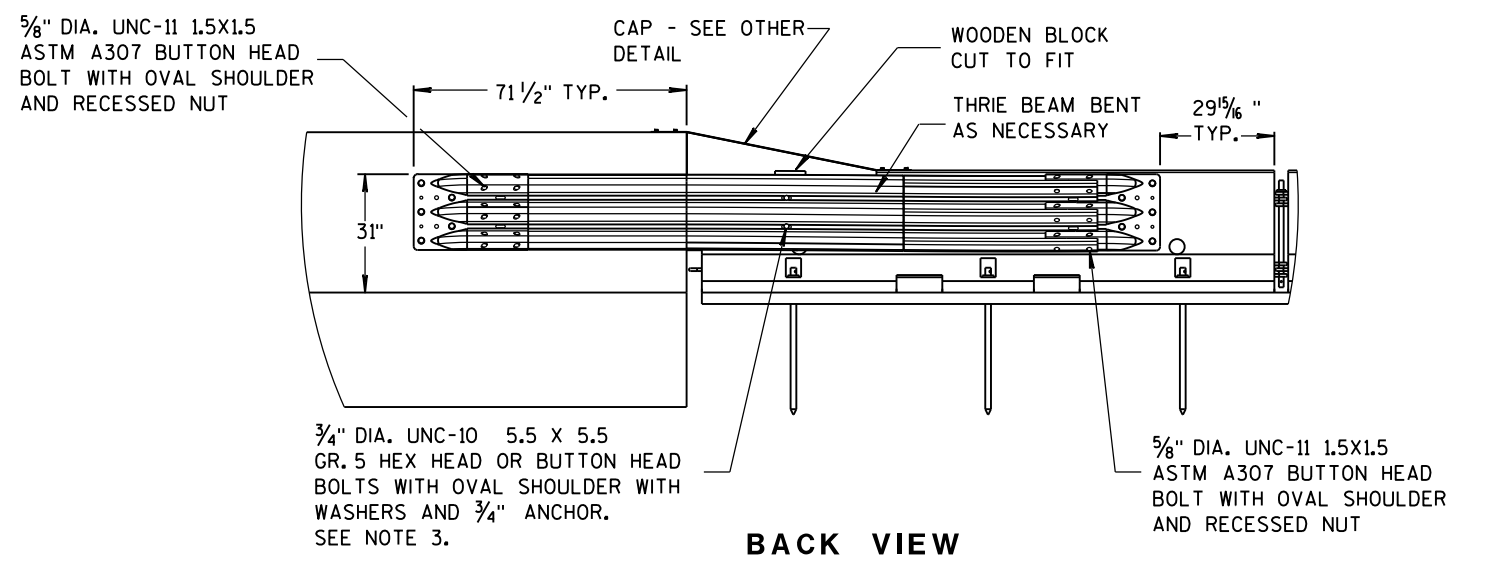


FRONT VIEW

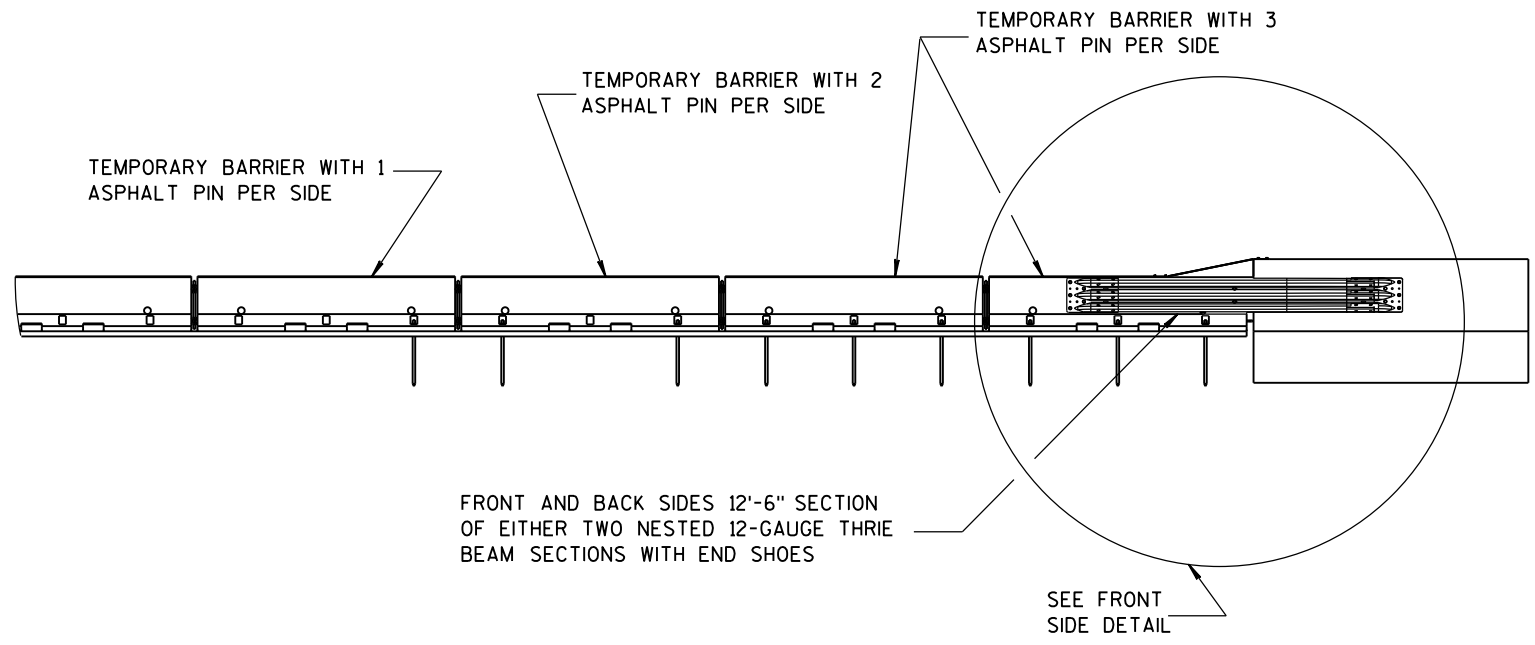


TEMPORARY BARRIER PLACEMENT FOR BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM

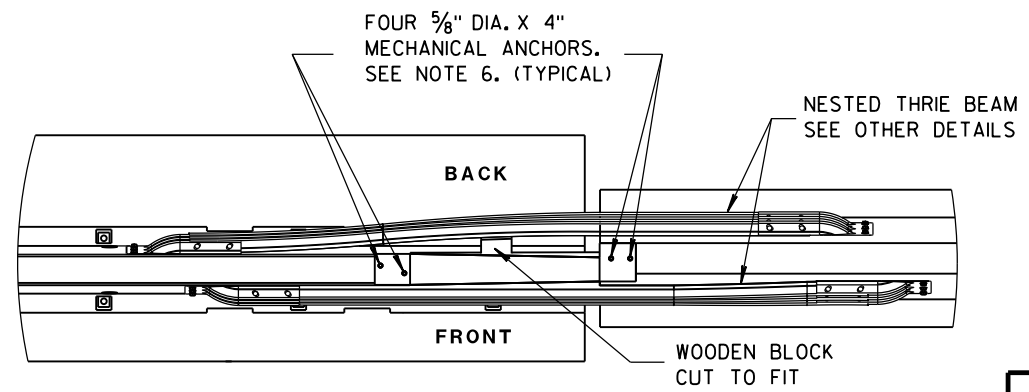
- NOTES**
- NESTED THRIE BEAM IS REQUIRED ON BOTH SIDES OF THE TEMPORARY BARRIER FOR ALL INSTALLATIONS REGARDLESS OF TRAFFIC.
- CAP END PLATE PLACED FLUSH WITH UPSTREAM END OF PERMANENT BARRIER OR PARAPET.
 - THRIE BEAM PIECES ARE OFFSET 15 1/4" TO PREVENT INTERFERENCE FROM THE ANCHORS ON OPPOSING SIDES.
 - MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 9.48 KIPS AND ULTIMATE SHEAR LOAD 10.48 KIPS.
 - MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 17.9 KIPS AND ULTIMATE SHEAR LOAD 21.96 KIPS.
 - MAY BE USED ON CONCRETE OR ASPHALT PAVEMENTS. ASPHALT OPTION SHOWN. FOR CONCRETE OPTION SEE OTHER DETAILS.
 - MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 12.14 KIPS AND ULTIMATE SHEAR LOAD 17.5 KIPS.



BACK VIEW



FRONT VIEW

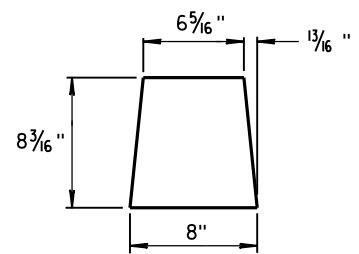


PLAN VIEW

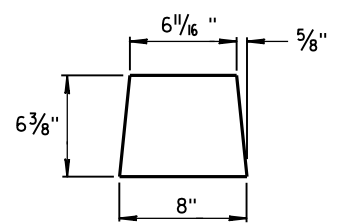
BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM

**CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"**

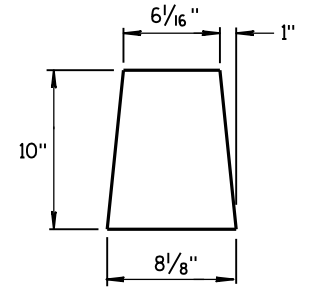
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



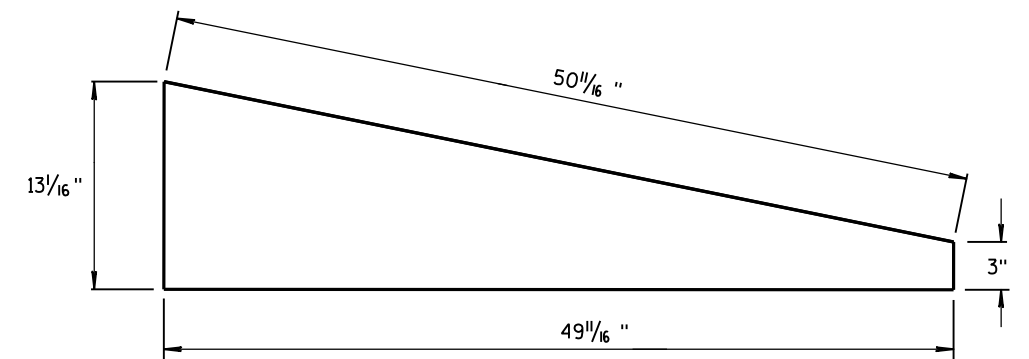
GUSSET 1



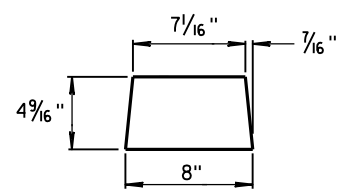
GUSSET 2



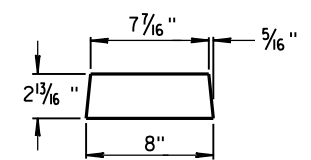
END PLATE



SIDE PLATE

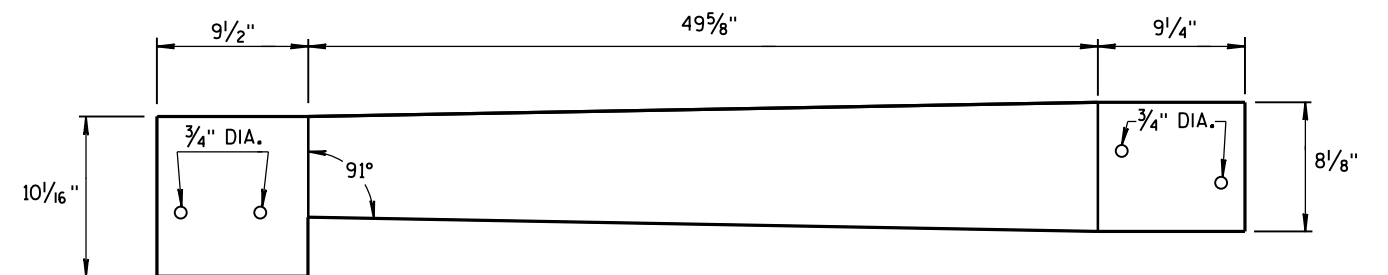


GUSSET 3

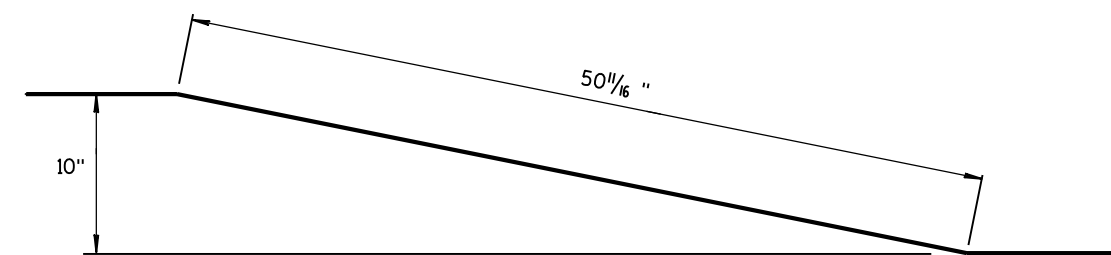


GUSSET 4

GUSSETS

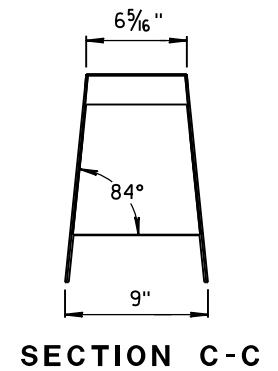
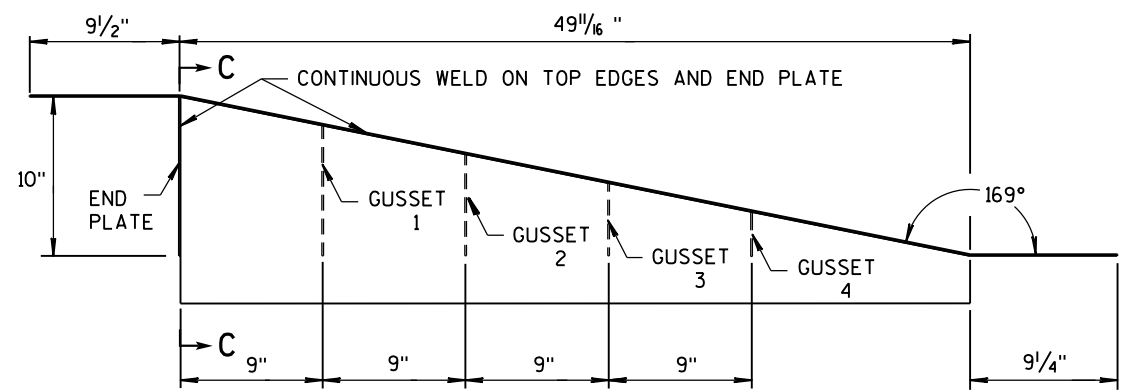
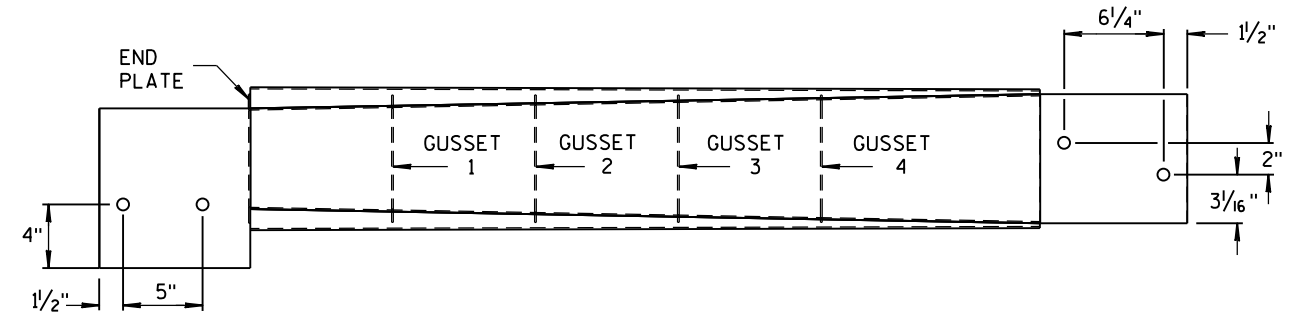


TOP PLATE



SIDE, TOP AND END PLATES FOR CAP FROM TEMPORARY CONCRETE BARRIER TO 42" PERMANENT CONCRETE BARRIER

SIDE PLATES, TOP PLATE, END PLATE AND GUSSETS ARE 12 GAUGE ASTM A36 GALVANIZED STEEL.



SECTION C-C

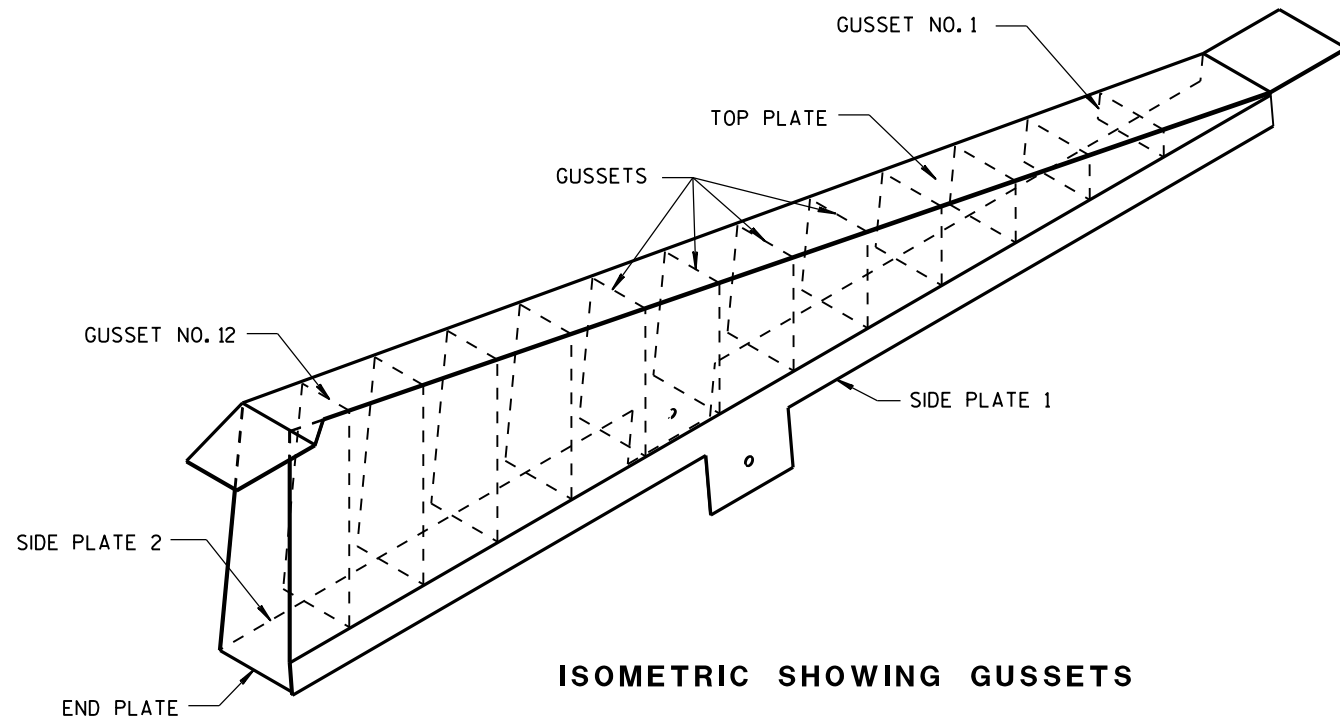
NOTES

- FOUR GUSSETS AND END PLATE ARE STITCH WELDED ON THREE SIDES.
- TWO TRIANGULAR SIDE PLATES ARE STITCH WELDED TO TOP PLATE, END PLATE, AND GUSSETS.

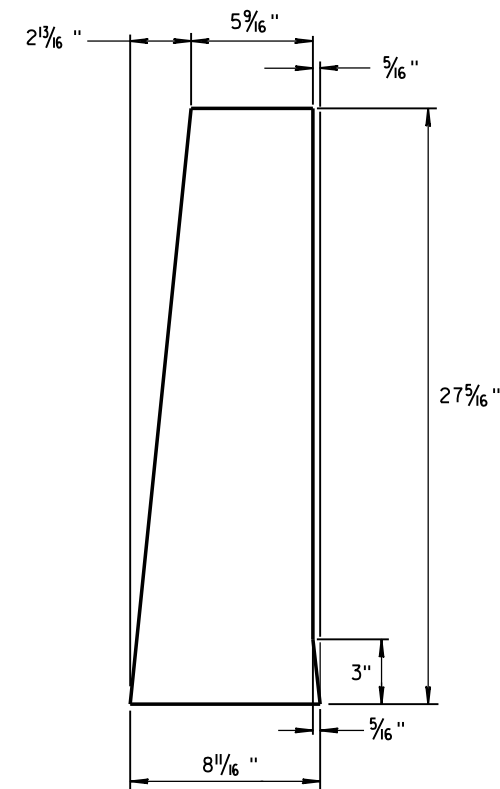
CAP DETAILS FOR TEMPORARY CONCRETE BARRIER TO 42" PERMANENT CONCRETE BARRIER

CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

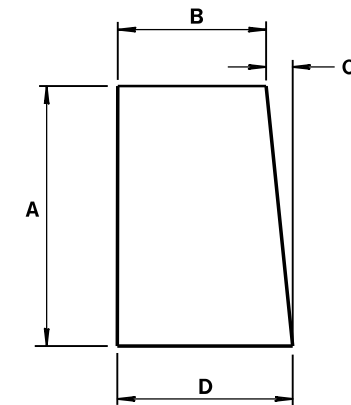


ISOMETRIC SHOWING GUSSETS



END PLATE

1/8" STEEL PLATE



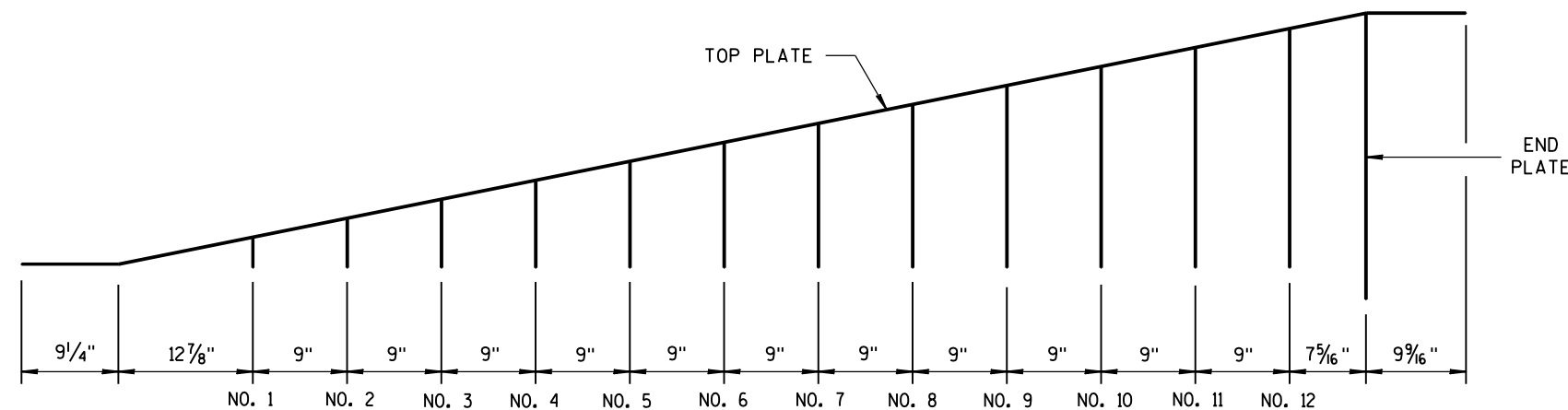
GUSSETS 1 - 12

ALL GUSSETS 1/8" STEEL PLATE

GUSSET DIMENSIONS				
GUSSET NO.	A	B	C	D
1	2 7/8"	7 3/4"	1/4"	8
2	4 1/16 "	7 7/16 "	1/2"	8
3	6 1/2"	7 3/8 "	11/16 "	8 1/16 "
4	8 5/16 "	7 3/16 "	7/8"	8 1/16 "
5	10 1/8"	7"	1 1/16 "	8 1/16 "
6	11 5/16 "	6 13/16 "	1 1/4"	8 1/16 "
7	13 3/4"	6 5/8 "	1 7/16 "	8 1/16 "
8	15 3/16 "	6 7/16 "	1 9/16 "	8 1/16 "
9	17 3/8"	6 1/4"	1 13/16 "	8 1/16 "
10	19 3/16 "	6 1/16 "	1 15/16 "	8 1/16 "
11	21"	5 7/8 "	2 3/16 "	8 1/16 "
12	22 13/16 "	5 11/16 "	2 5/16 "	8 1/16 "

SIDE PLATES, TOP PLATE, END PLATE AND GUSSETS ARE 12 GAUGE ASTM A36 STEEL AND GALVANIZED.

GUSSETS AND END PLATE ARE STITCH WELDED ON 3 SIDES. TWO TRIANGULAR SIDE PLATES ARE STITCH WELDED TO TOP PLATE, END PLATE AND GUSSETS.

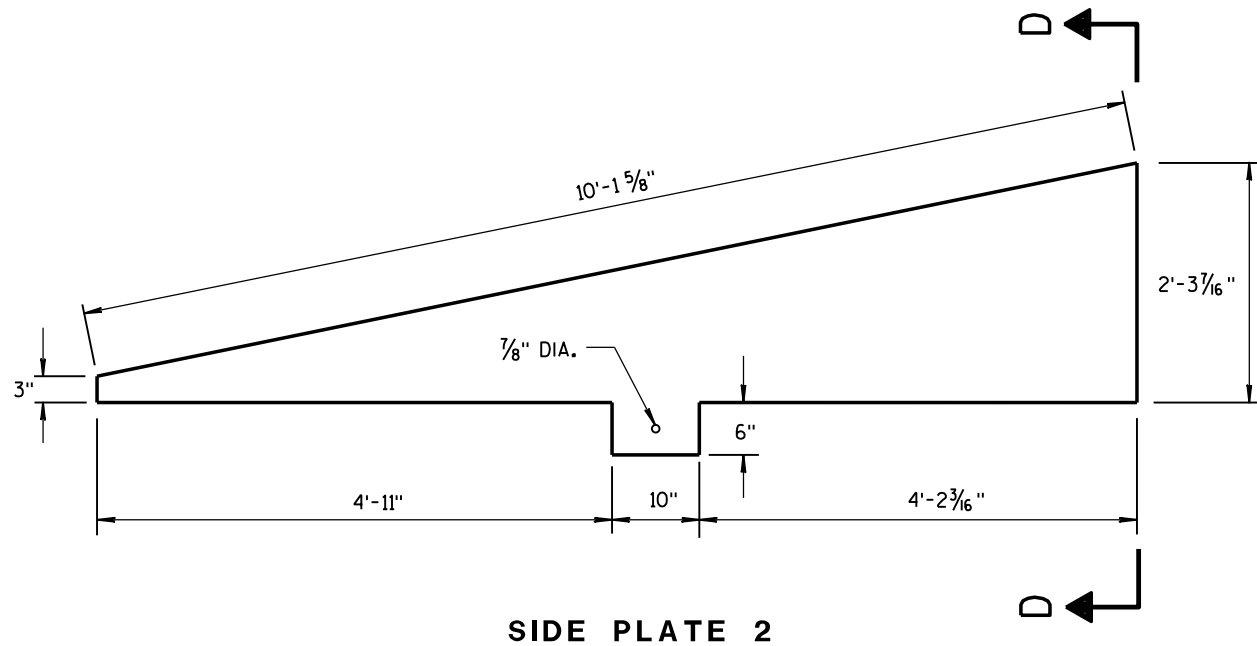


GUSSET LOCATION

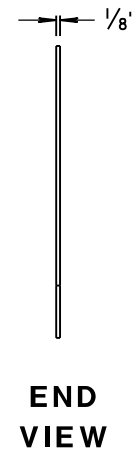
CAP DETAILS FOR TEMPORARY CONCRETE BARRIER TO 56" PERMANENT CONCRETE BARRIER

CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

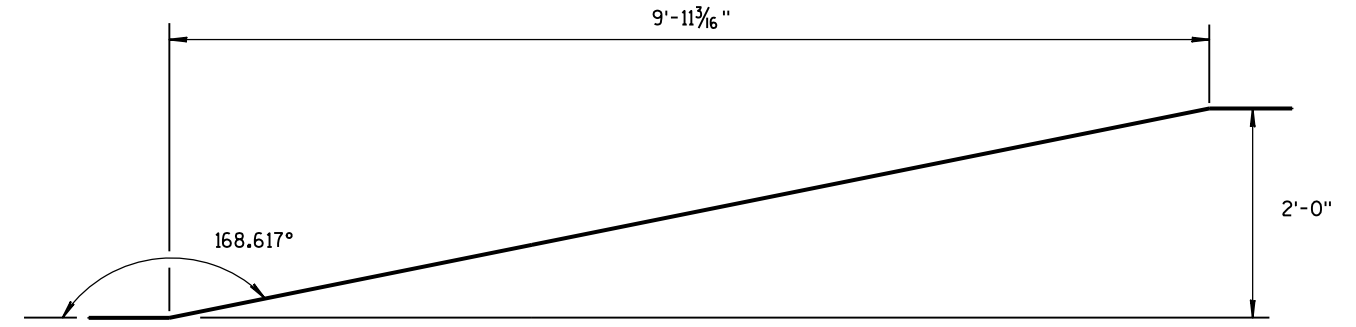
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



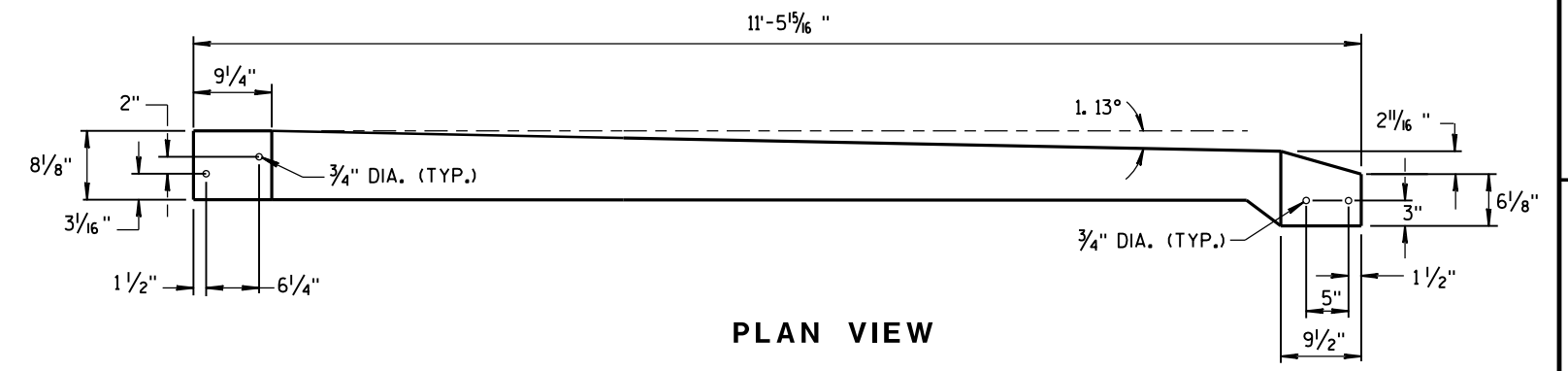
SIDE PLATE 2



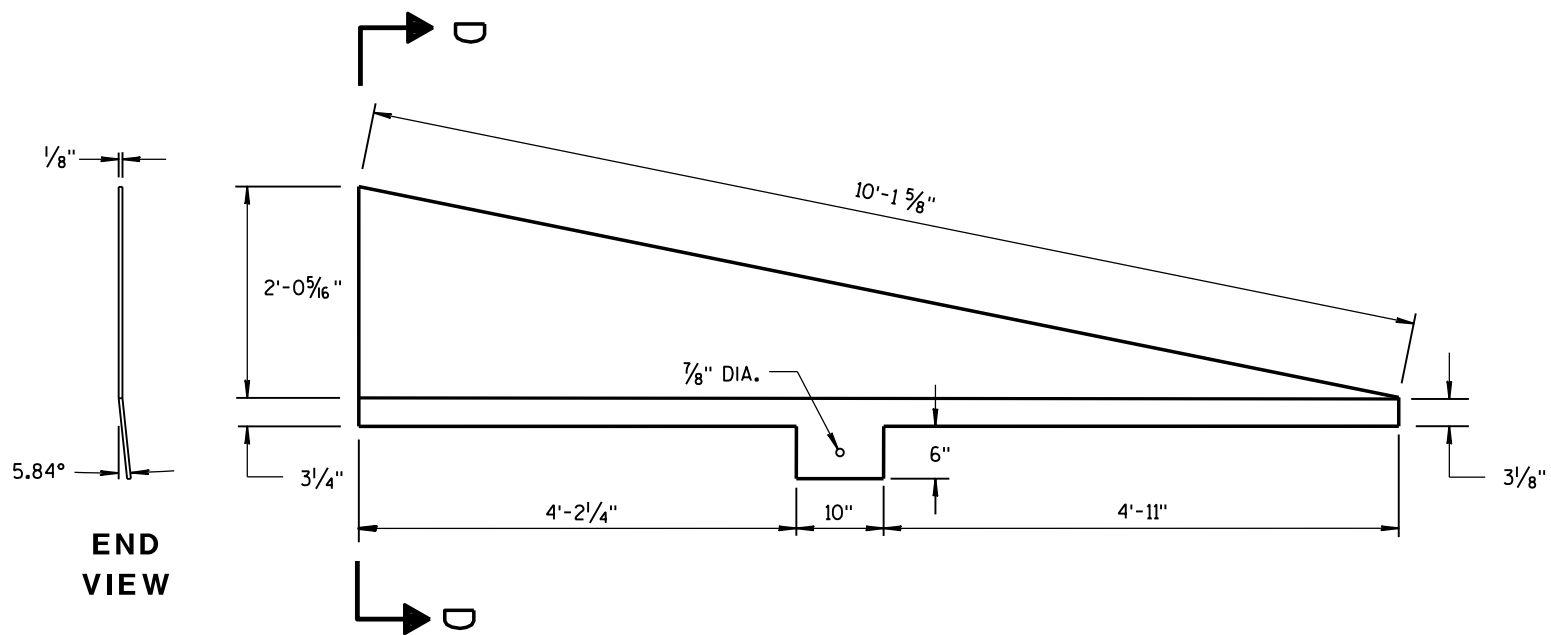
END VIEW



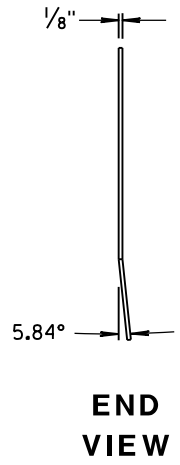
SIDE VIEW
TOP PLATE



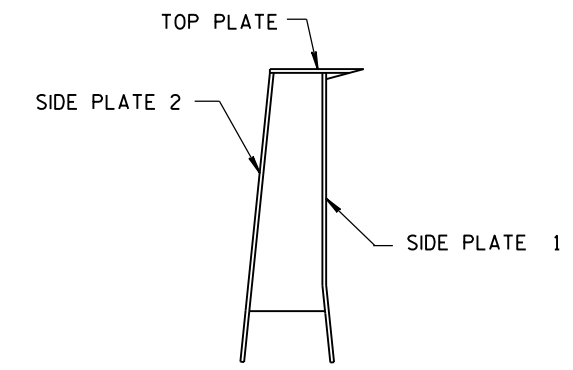
PLAN VIEW
TOP PLATE



SIDE PLATE 1



END VIEW



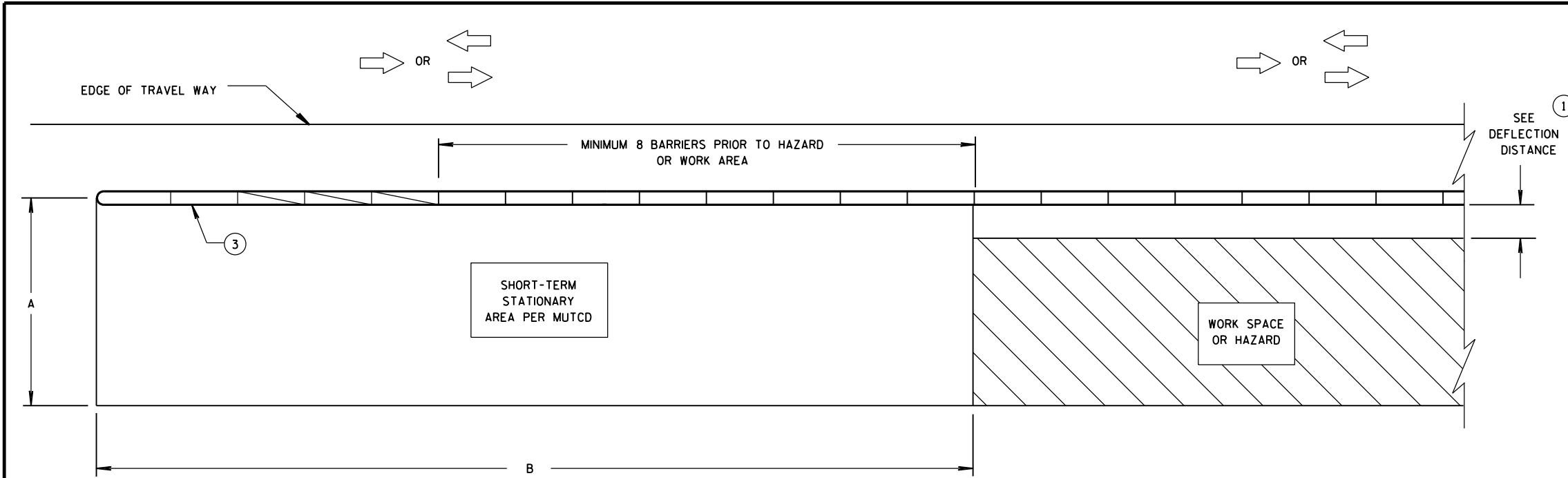
SECTION D-D

CAP DETAILS FOR TEMPORARY CONCRETE BARRIER TO 56" PERMANENT CONCRETE BARRIER

**CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Rodney Taylor
DATE ROADWAY STANDARD DEVELOPMENT
FHWA UNIT SUPERVISOR



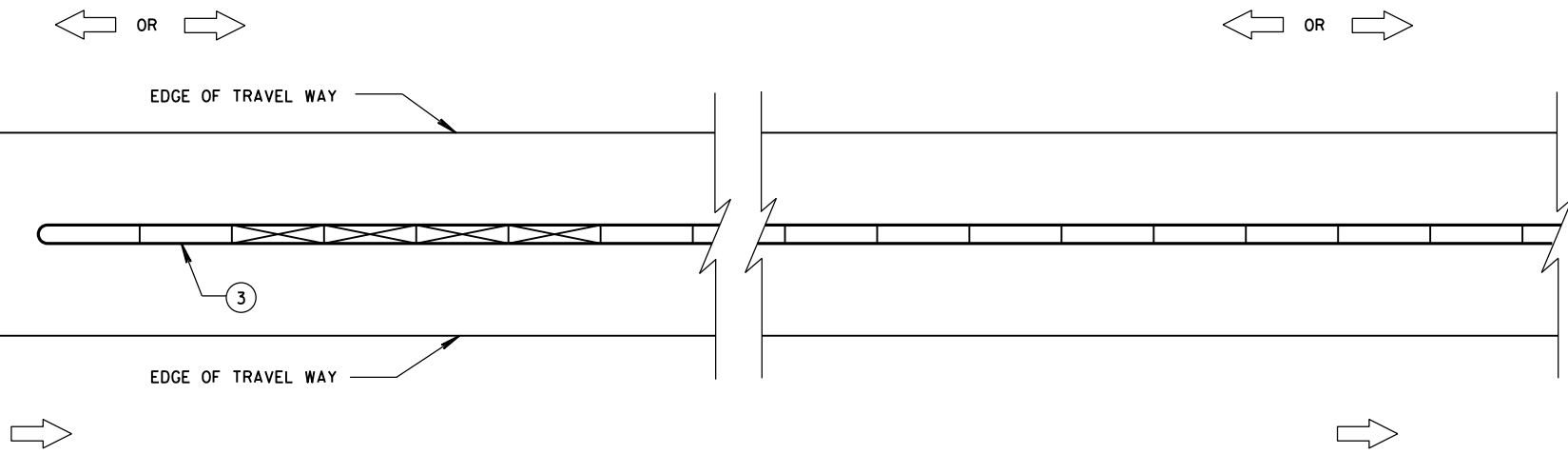
DIMENSION A TABLE ②

FACILITY	POSTED SPEED MPH	DIMENSION A	
		MIN. FT	MAX. FT
FREEWAY/EXPRESSWAY	ALL	15	20
NON-FREEWAY/EXPRESSWAY	GREATER THAN OR EQUAL TO 45	10	15
NON-FREEWAY/EXPRESSWAY	LESS THAN 45	8	10
AADT LESS THAN 1,500	ALL	8	10

**CRASH CUSHION/SAND BARREL ARRAY AND TEMPORARY BARRIER
INSTALLATION FOR TRAFFIC ON ONE SIDE OF BARRIER**

DIMENSION B TABLE ②

POSTED SPEEDS MPH	DIMENSION B FT
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645



**CRASH CUSHION/SAND BARREL ARRAY AND TEMPORARY BARRIER
INSTALLATION FOR TRAFFIC ON BOTH SIDES OF BARRIER**

LEGEND

- DIRECTION OF TRAVEL
- CRASH CUSHION OR SAND BARREL ARRAY
- SEE FREE STANDING TRANSITION TO TIED-DOWN SYSTEM DETAILS
- SEE BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM DETAILS
- 3 PINS PLACED ON TRAFFIC SIDE OF BARRIER
- PERMANENT CONCRETE BARRIER OR CONCRETE PARAPET
- FREE STANDING TEMPORARY BARRIER

GENERAL NOTES

SEE STANDARD DETAIL DRAWING 14B7 FOR MORE INFORMATION.

DETAILS PROVIDE A GENERAL LAYOUT OF TEMPORARY CONCRETE BARRIER, CRASH CUSHIONS, SAND BARREL ARRAYS AND TIE DOWN TRANSITIONS. DETAILS PROVIDED MAY NOT FIT ALL POSSIBLE SITUATIONS OR SITE CONDITIONS. SEE OTHER SECTIONS OF THE CONTRACT OR PROJECT ENGINEER FOR MORE DETAILS.

ADDITIONAL TEMPORARY BARRIER MAY BE REQUIRED TO PROTECT TRAVELING PUBLIC FROM HAZARDS, CONTRACTOR'S OPERATIONS OR TO CONTROL TRAFFIC.

TEMPORARY BARRIER MAY BE REQUIRED TO BE ANCHORED TO PAVEMENT OR BRIDGE DECK.

FOR DETAILS ON CRASH CUSHION OR SAND BARREL ARRAYS SEE OTHER SECTIONS OF THE PLAN AND MANUFACTURE'S DETAILS.

SLOPES LEADING TO TEMPORARY BARRIER, CRASH CUSHION OR SAND BARREL ARRAY ARE 10:1 OR LESS.

- ① FOR DEFLECTION INFORMATION SEE STANDARD DETAIL DRAWING 14B7.
- ② VALUES PROVIDED MAY NOT FIT ALL POSSIBLE SITUATIONS OR SITE CONDITIONS. SEE OTHER SECTIONS OF THE CONTRACT OR PROJECT ENGINEER FOR MORE DETAILS.
- ③ ANCHOR TEMPORARY BARRIER ACCORDING TO CRASH CUSHION OR SAND BARREL MANUFACTURER'S RECOMMENDATIONS. IF MANUFACTURER'S RECOMMENDATIONS ARE NOT PROVIDED, ANCHOR 3 PINS ON TRAFFIC SIDE.

**CRASH CUSHION/SAND BARREL
ARRAY AND OTHER TEMPORARY
BARRIER LAYOUT DETAILS**

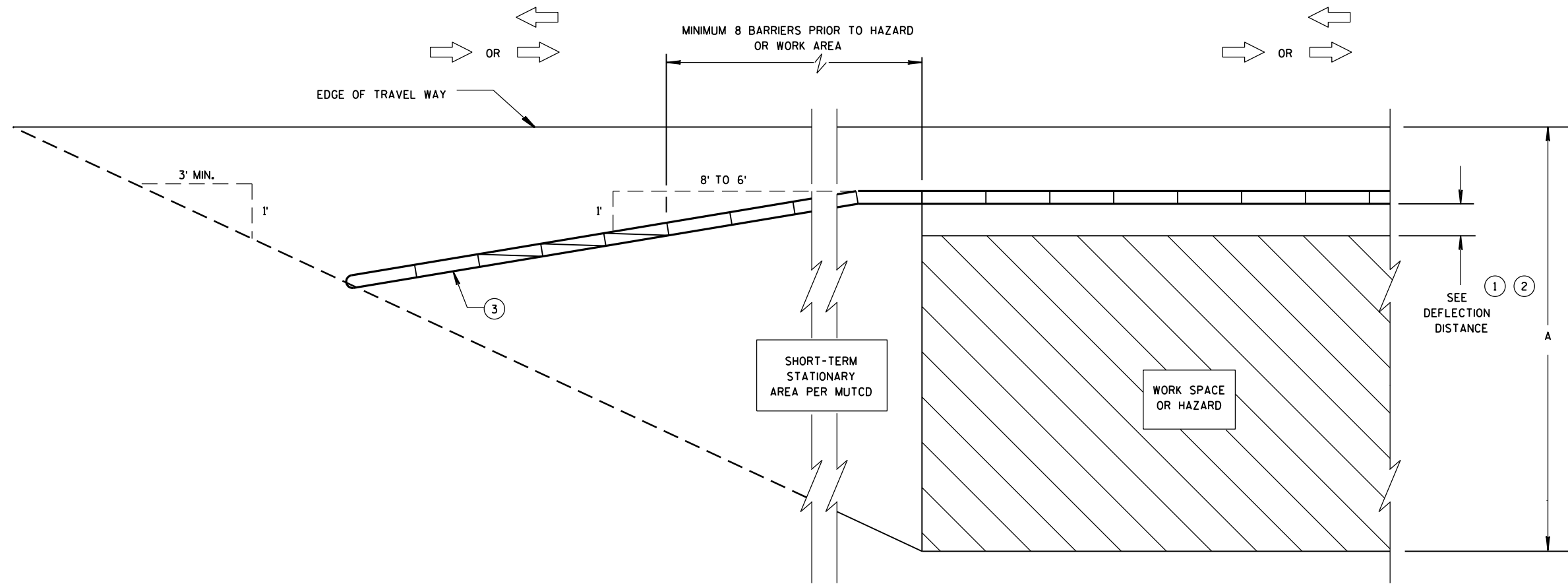
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

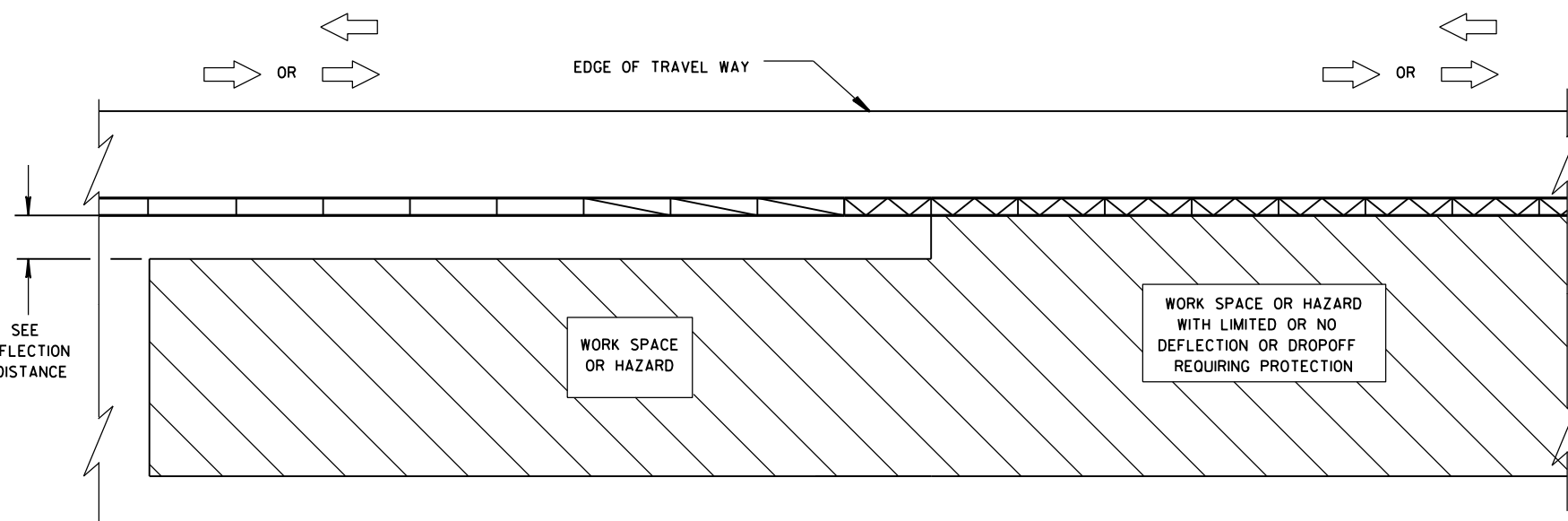
6

S.D.D. 14 B 8-2a

S.D.D. 14 B 8-2a



**CRASH CUSHION/SAND BARREL ARRAY AND TEMPORARY BARRIER
INSTALLATION FOR TRAFFIC ON ONE SIDE - FLARED INSTALLATION**



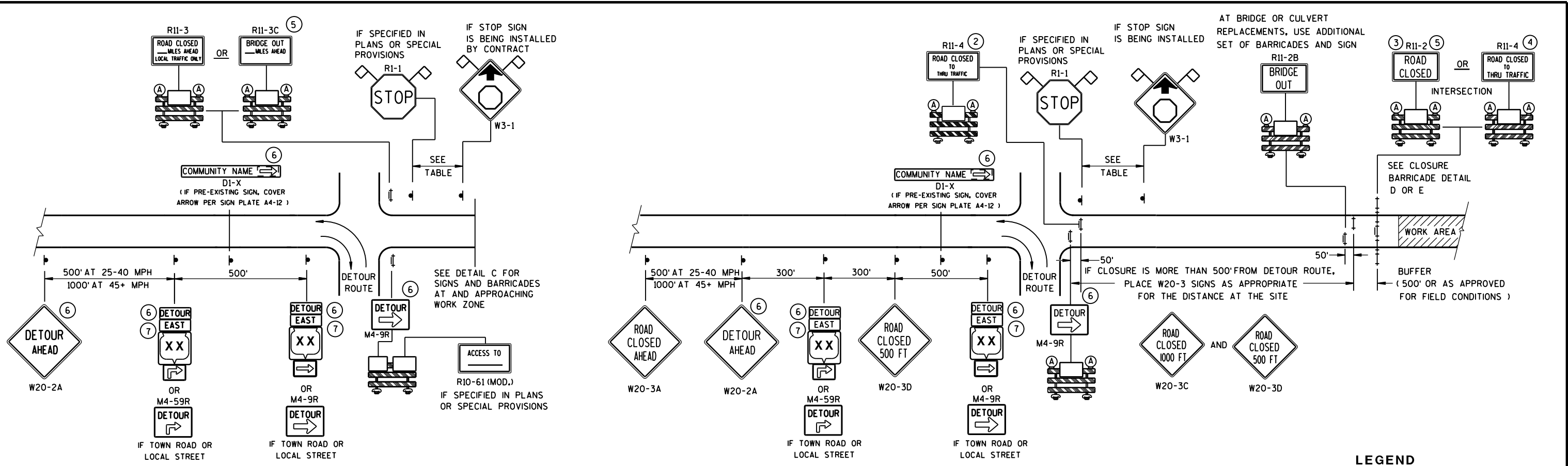
**TRANSITION FROM FREE STANDING TEMPORARY BARRIER
TO ANCHORED BARRIER**

LEGEND

- DIRECTION OF TRAVEL
- CRASH CUSHION OR SAND BARREL ARRAY
- SEE FREE STANDING TRANSITION TO TIED-DOWN SYSTEM DETAILS
- SEE BI-DIRECTIONAL TRANSITION TO TIED-DOWN SYSTEM DETAILS
- 3 PINS PLACED ON TRAFFIC SIDE OF BARRIER
- PERMANENT CONCRETE BARRIER OR CONCRETE PARAPET
- FREE STANDING TEMPORARY BARRIER

**CRASH CUSHION/SAND BARREL
ARRAY AND OTHER TEMPORARY
BARRIER LAYOUT DETAILS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

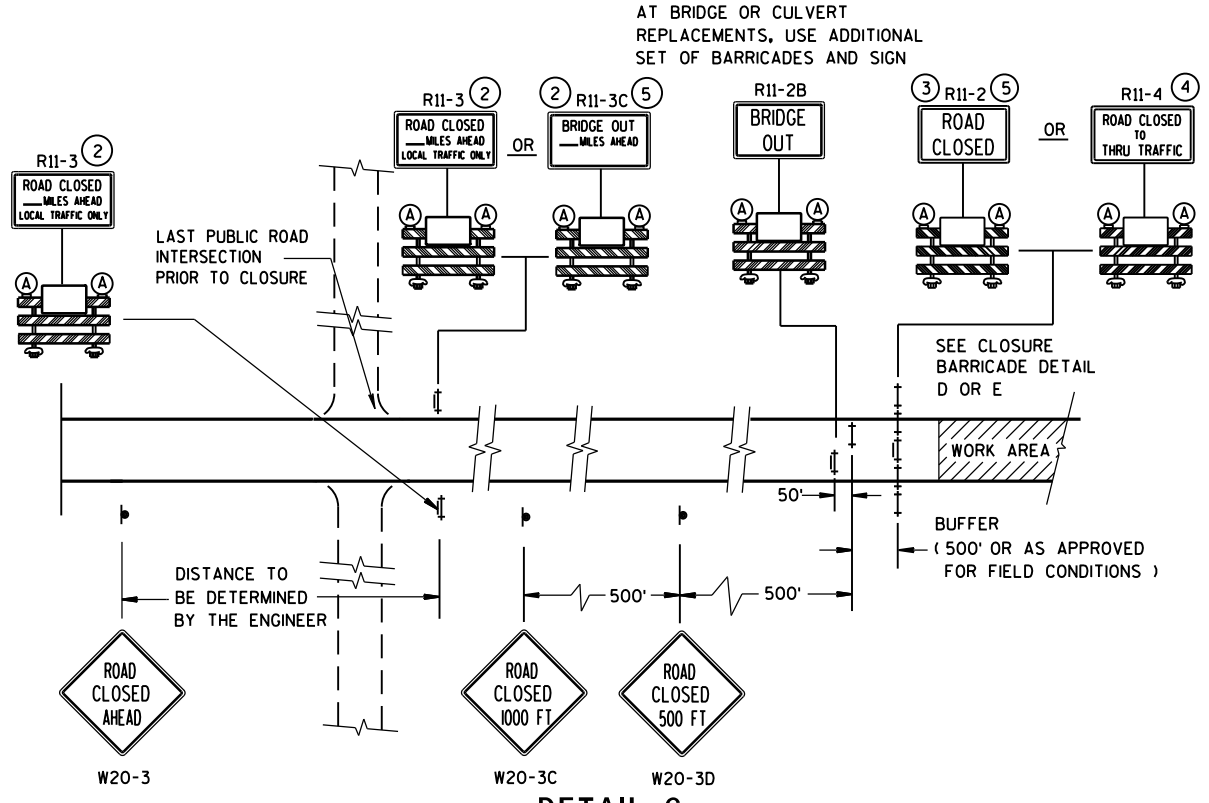


DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
 WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- ⊙ SIGN ON PERMANENT SUPPORT
 - ⊥ TYPE III BARRICADE
 - ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
 - Ⓐ TYPE "A" WARNING LIGHT (FLASHING)
 - ▨ WORK AREA
 - DETOUR EAST M4-8 M3-X
 - XX OR COUNTY XX OR XX M1-4 M1-5A M1-6
 - OR M05-1 M06-1
 - ◇ FLAGS, 16" X 16" MIN., (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750



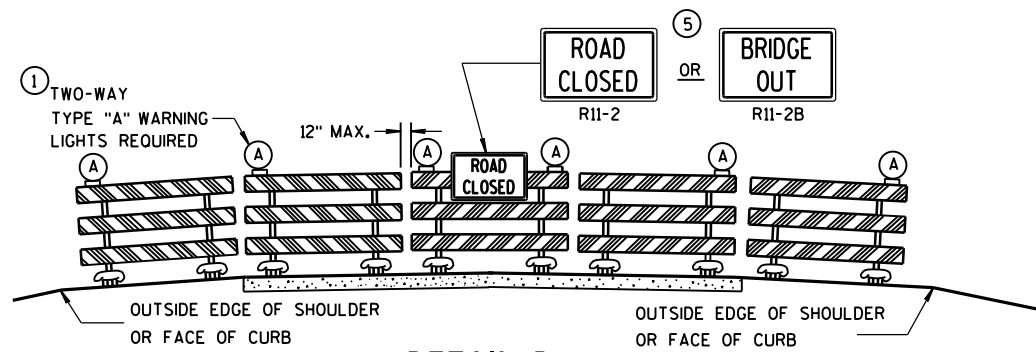
DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SEE SDD 15C2-SHEET "b"
 FOR GENERAL NOTES
 AND FOOTNOTES ① THROUGH ⑦

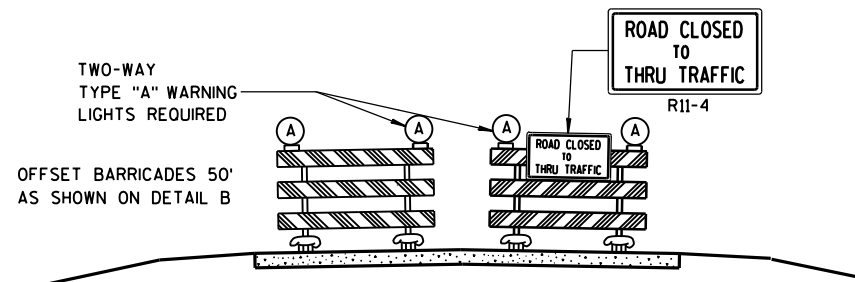
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

Sept. 2015 /S/ Peter Amakobe Atepe
 DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
 FHWA



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
 APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
 APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".

M4-9 SHALL BE 30" X 24".

M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)

M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)

M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)

M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)

D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

R1-1 SHALL BE 36" X 36".

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Sept. 2015 DATE	/S/ Peter Amokobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

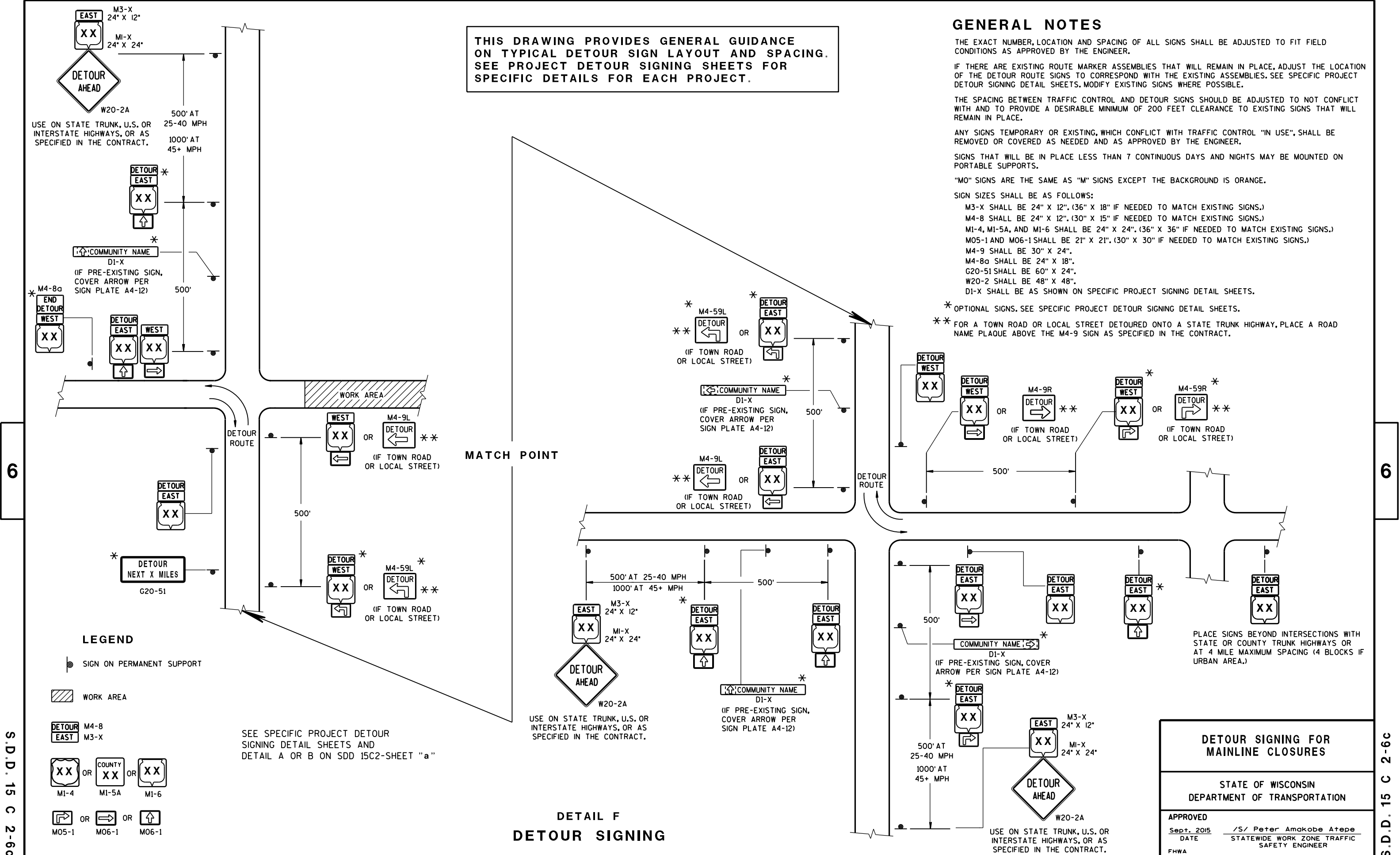
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-9 SHALL BE 30" X 24".
- M4-8a SHALL BE 24" X 18".
- G20-51 SHALL BE 60" X 24".
- W20-2 SHALL BE 48" X 48".
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



6

6

S.D.D. 15 C 2-6C

S.D.D. 15 C 2-6C

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

Sep. 2015 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

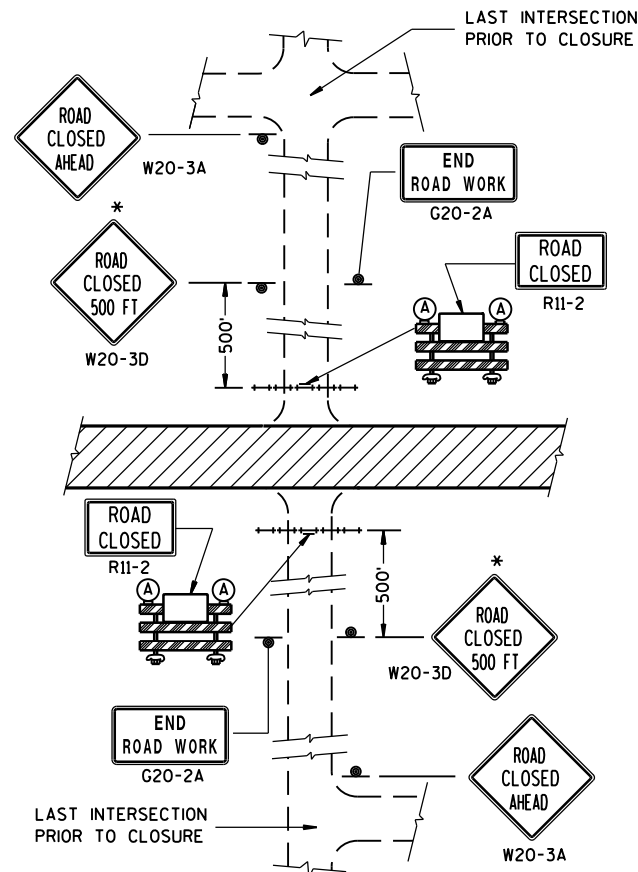
FHWA

**DETAIL F
DETOUR SIGNING**

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD 15C2-SHEET "a"

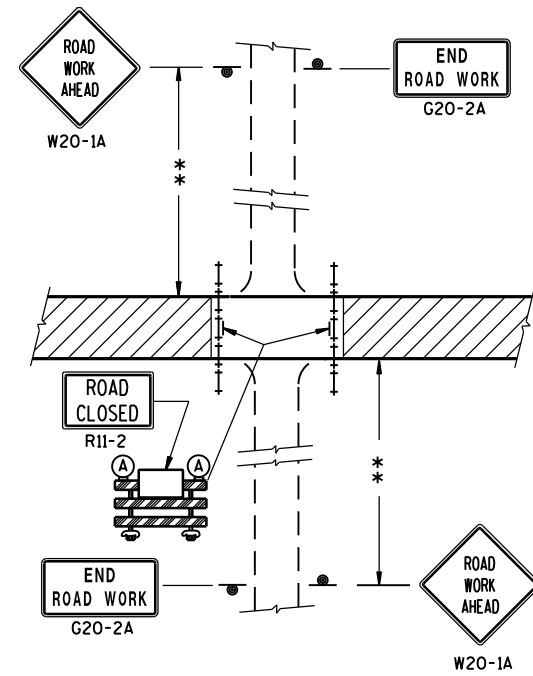
USE ON STATE TRUNK, U.S. OR INTERSTATE HIGHWAYS, OR AS SPECIFIED IN THE CONTRACT.

USE ON STATE TRUNK, U.S. OR INTERSTATE HIGHWAYS, OR AS SPECIFIED IN THE CONTRACT.



DETAIL 1

(NO ACCESS TO PROJECT)



DETAIL 2

(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT).

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

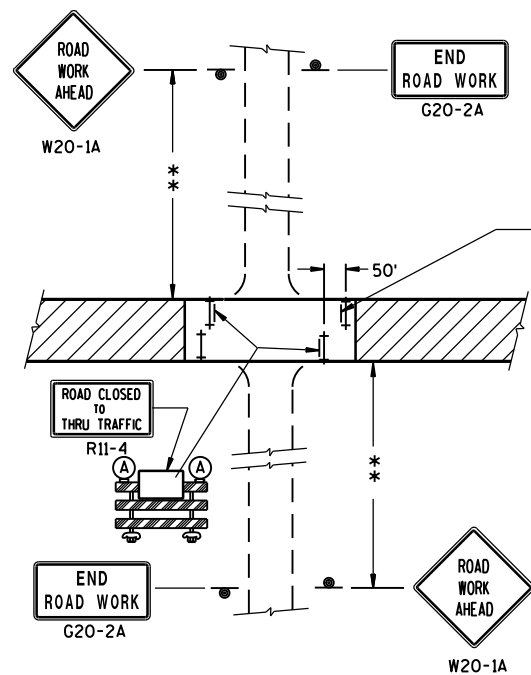
TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

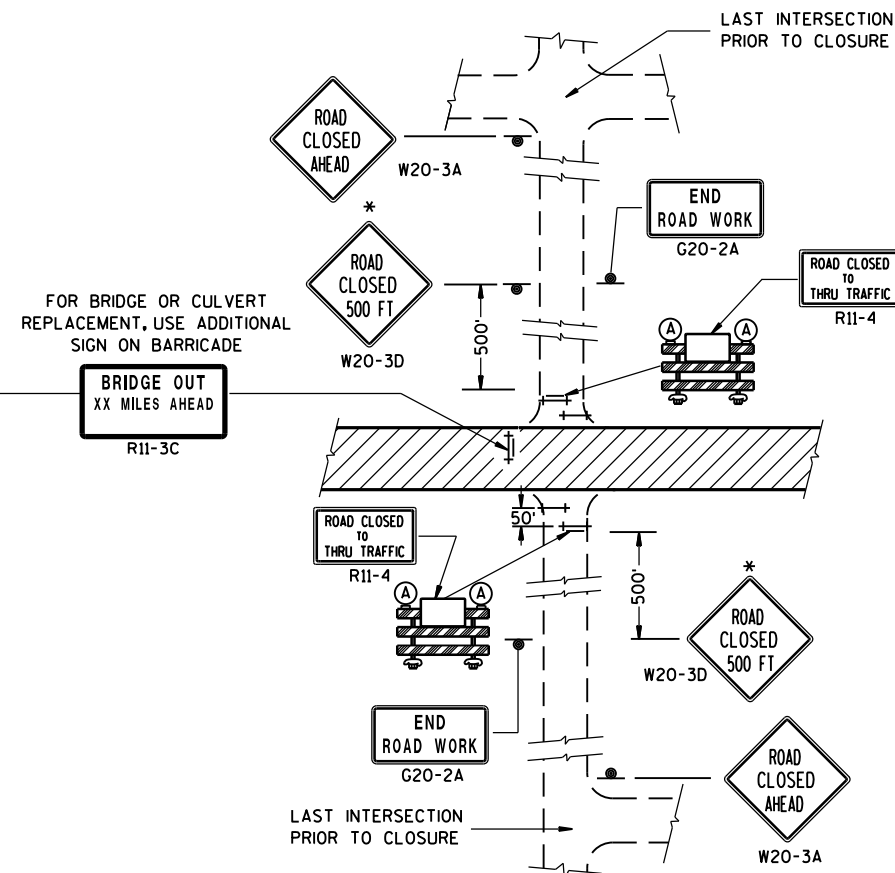
R11-2 SHALL BE 48" X 30".

R11-4 AND R11-3 SHALL BE 60" X 30".



DETAIL 3

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR,
LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

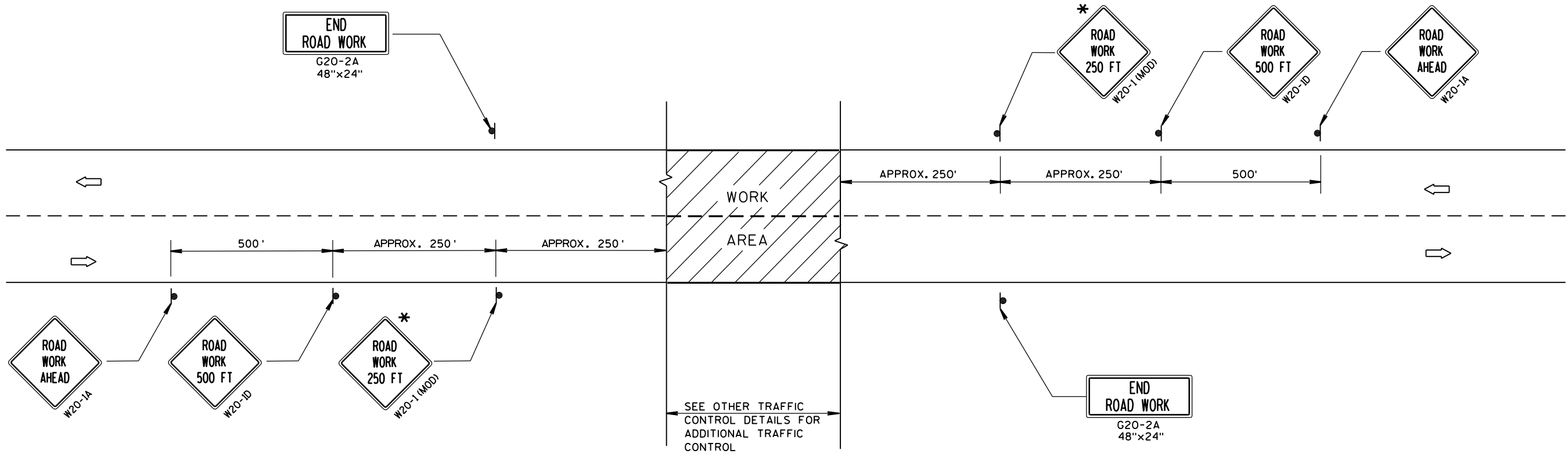
LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- Ⓐ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2017 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

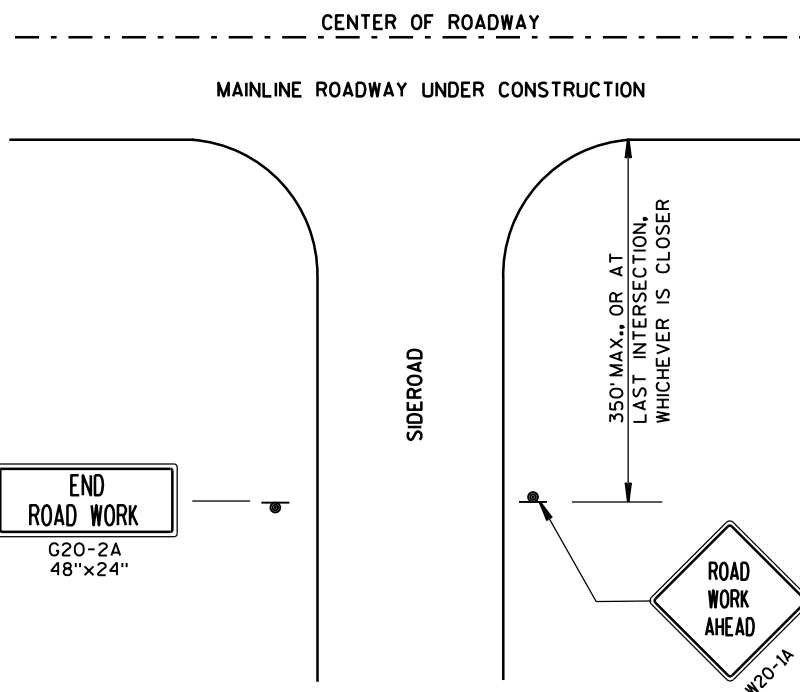
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



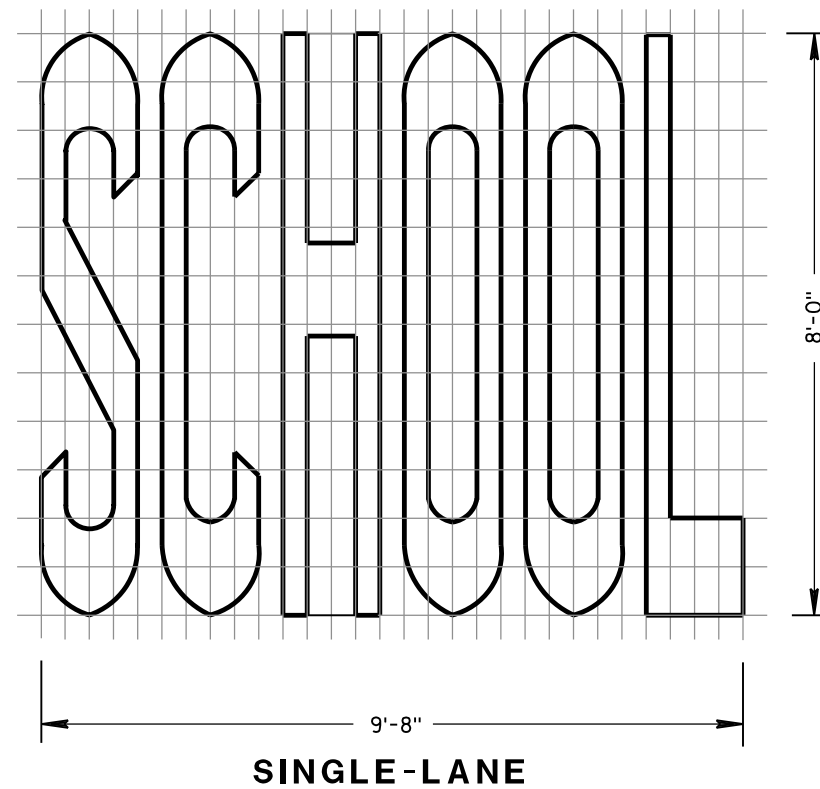
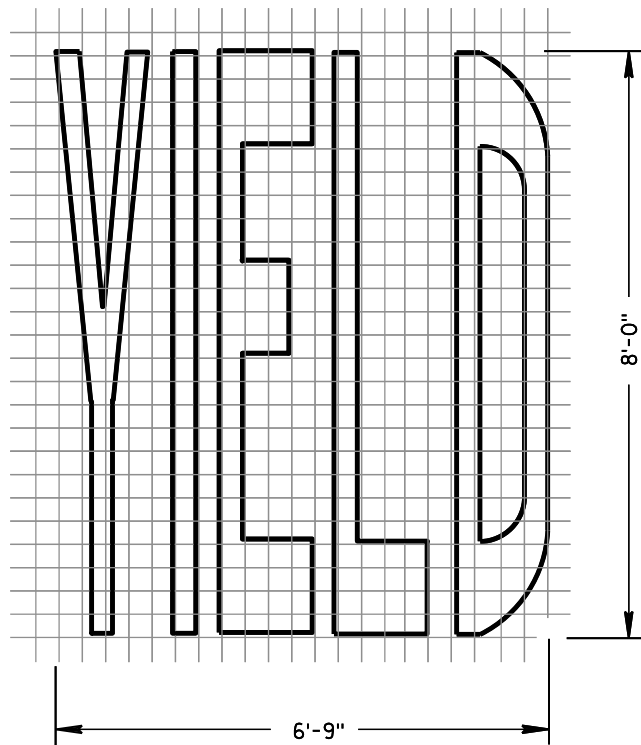
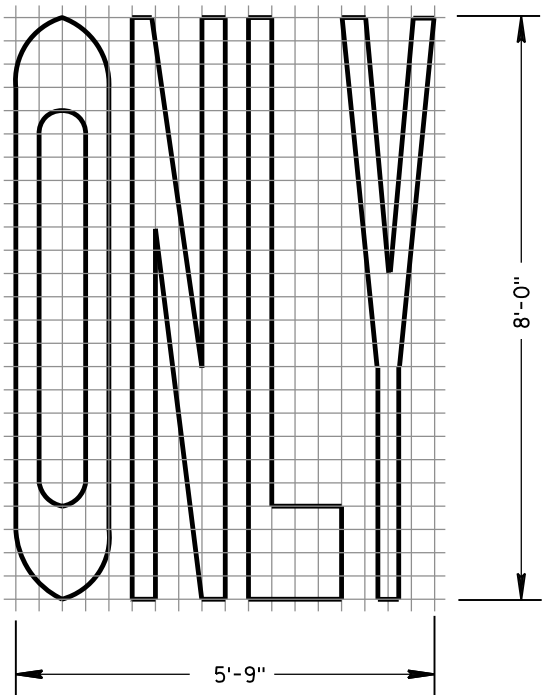
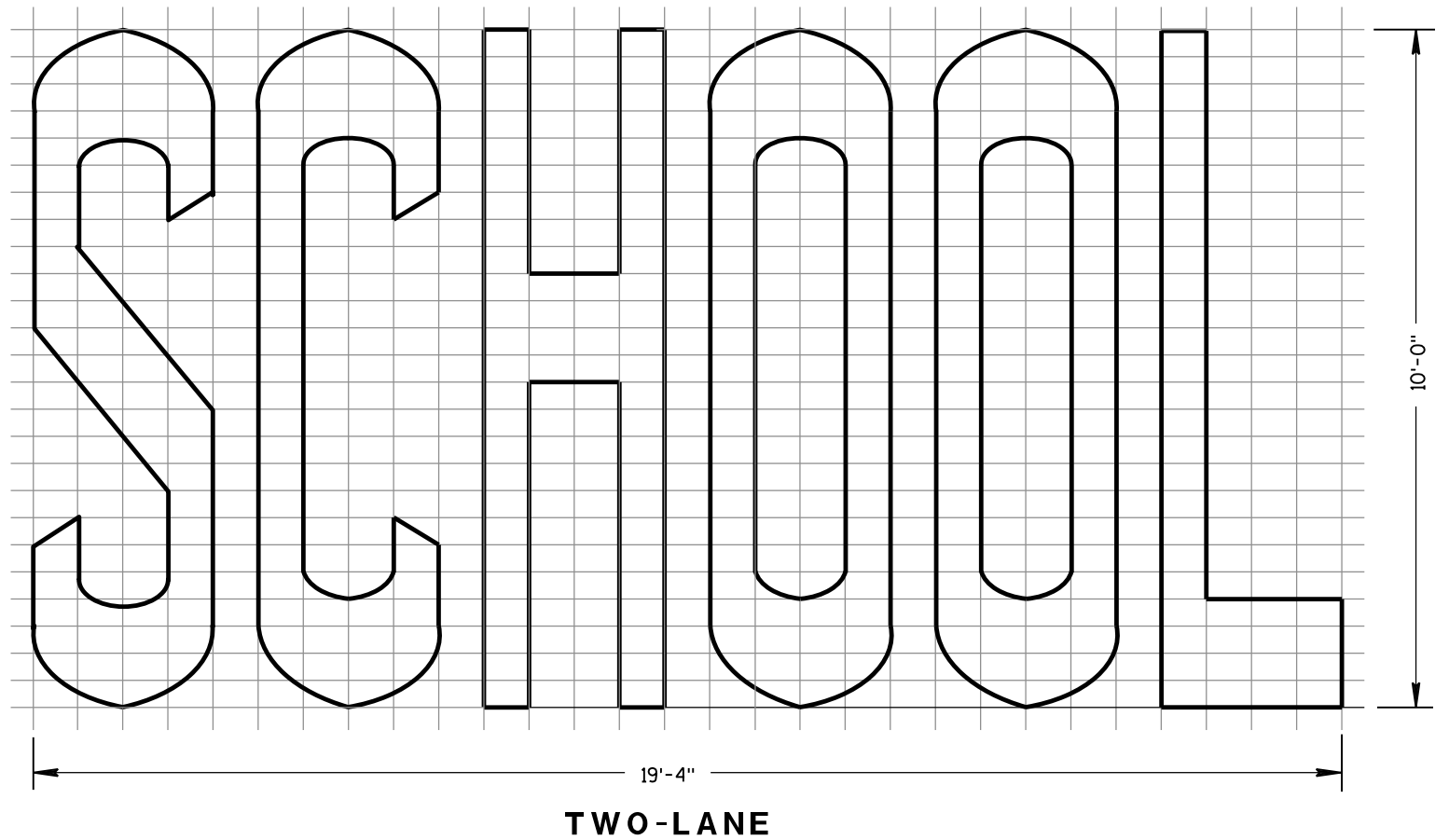
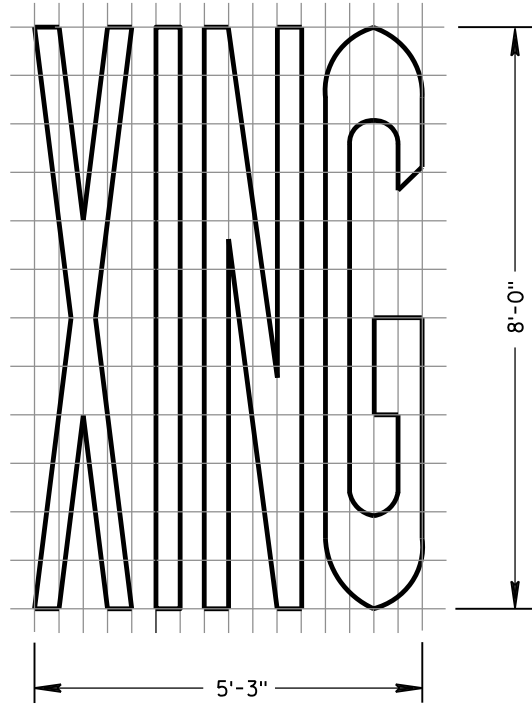
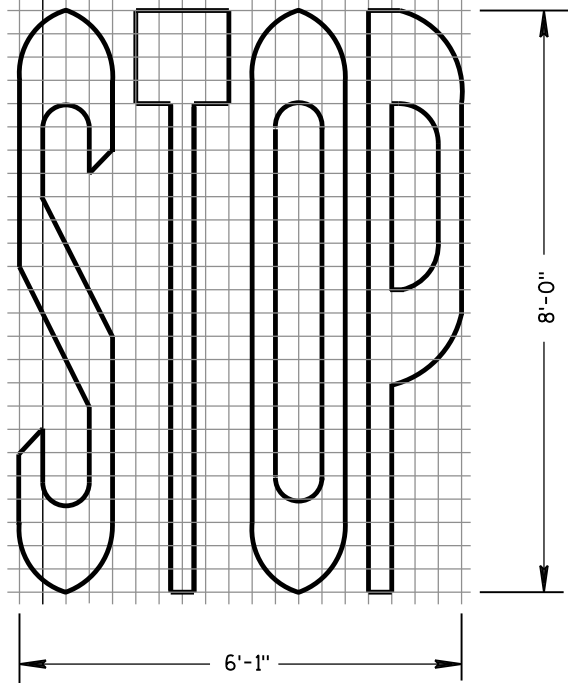
LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



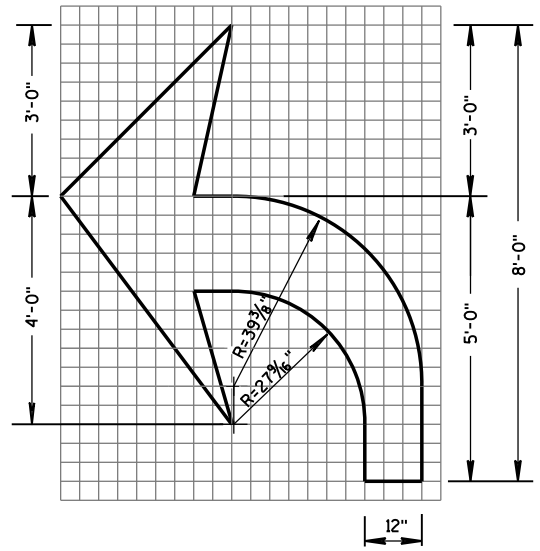
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6

S.D.D. 15 C 7-14b

S.D.D. 15 C 7-14b

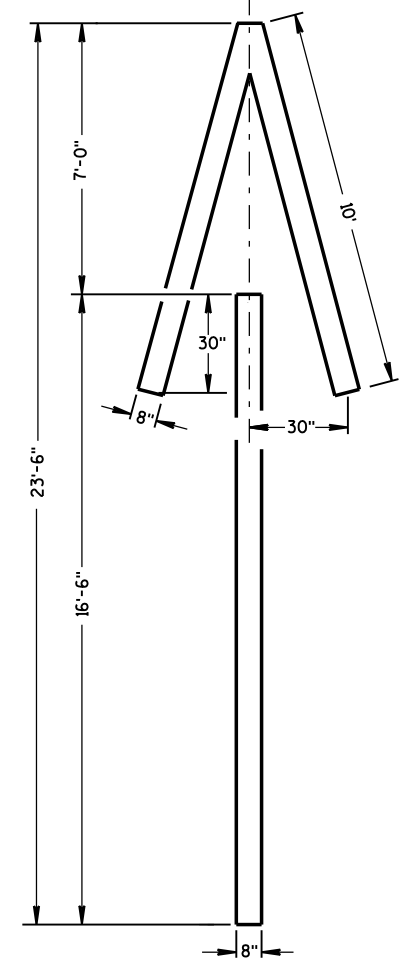
PAVEMENT MARKING WORDS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Matthew R. Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	



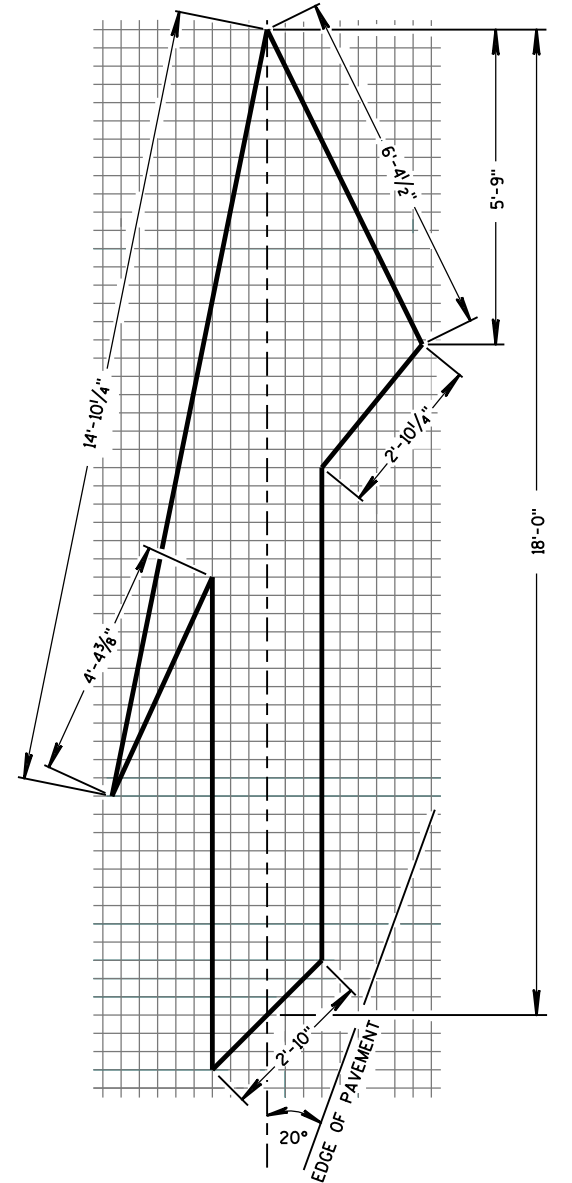
TYPE 2



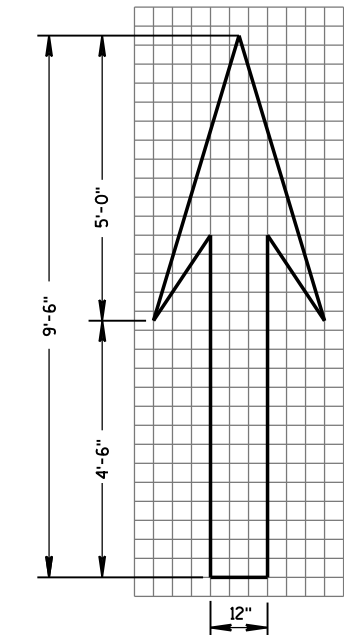
TYPE 6



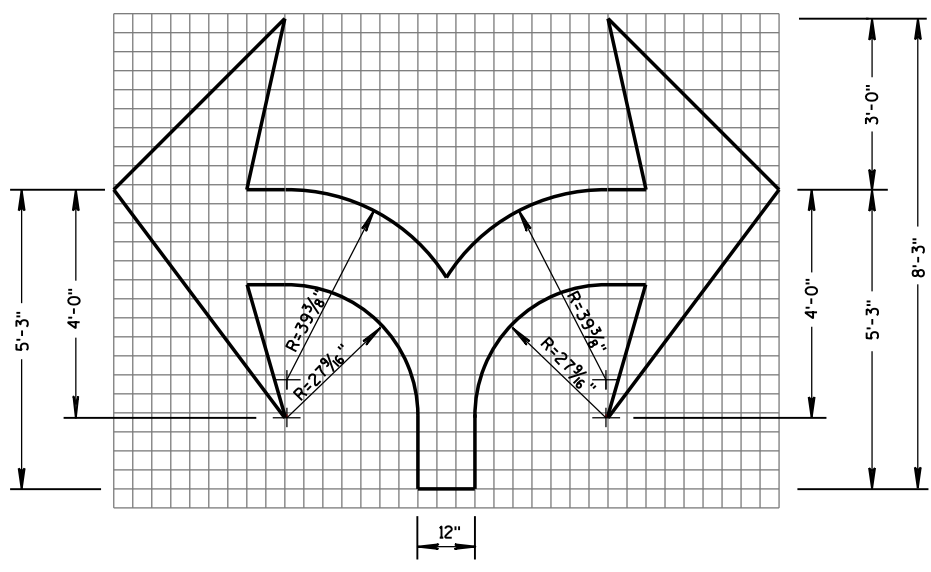
TYPE 4



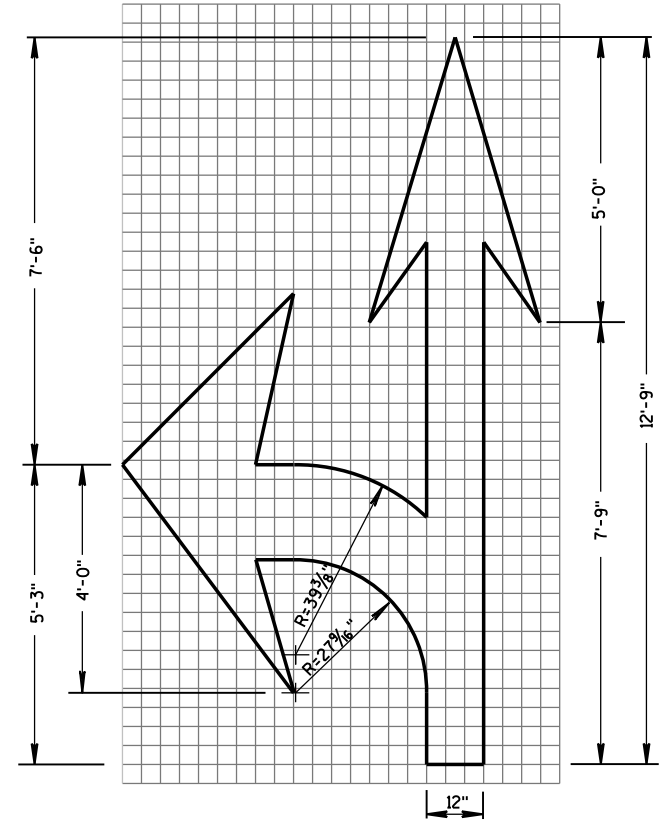
TYPE 5 LANE DROP ARROW



TYPE 1



TYPE 7

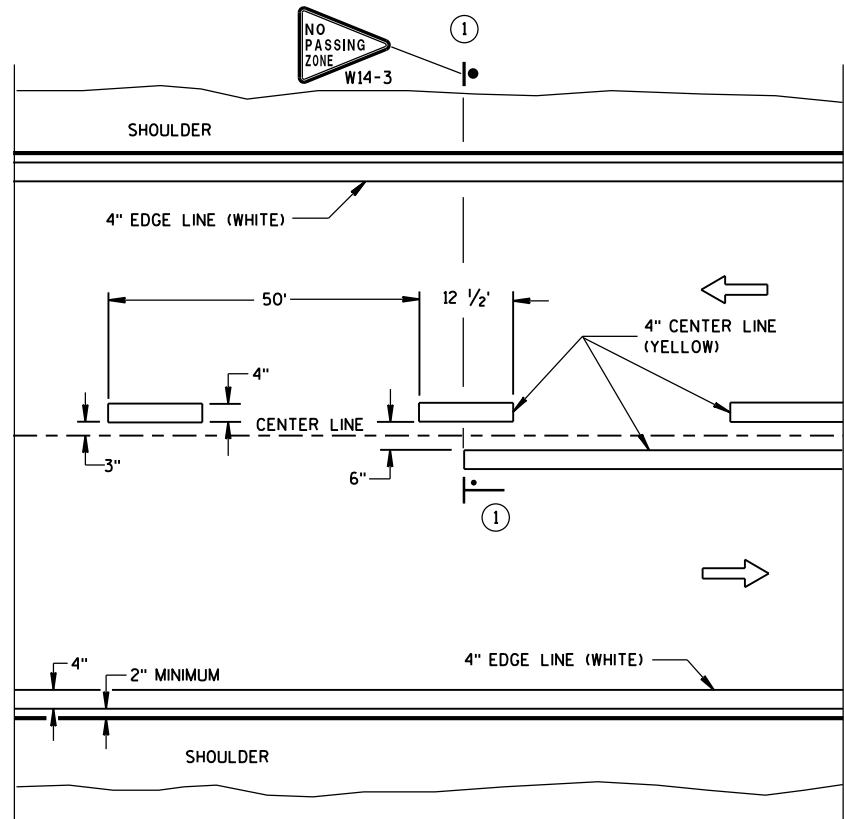


TYPE 3

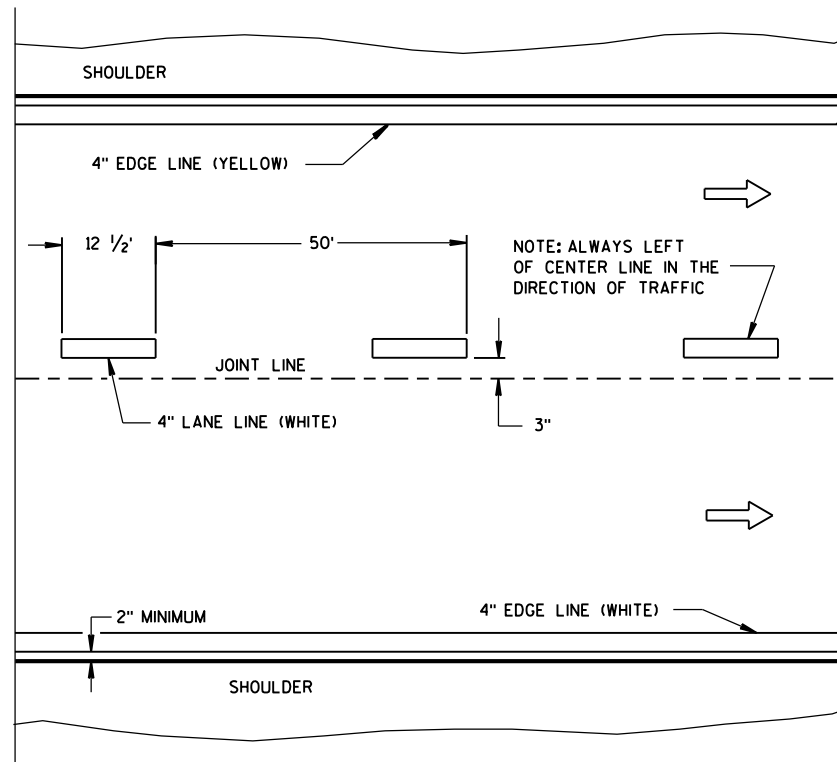
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/s/ Matthew R. Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

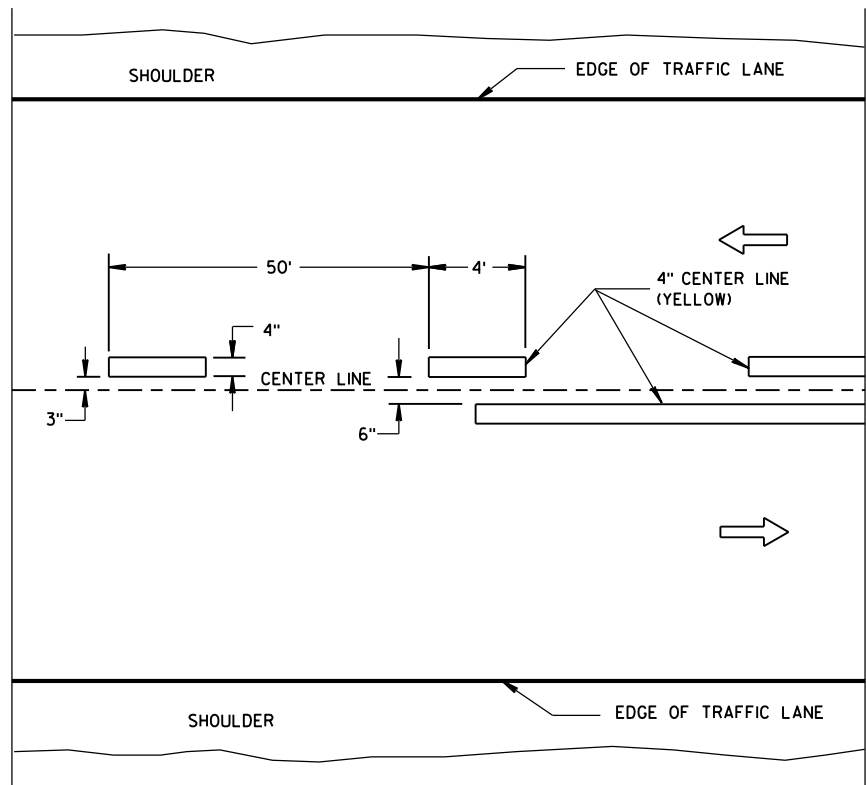


TWO WAY TRAFFIC

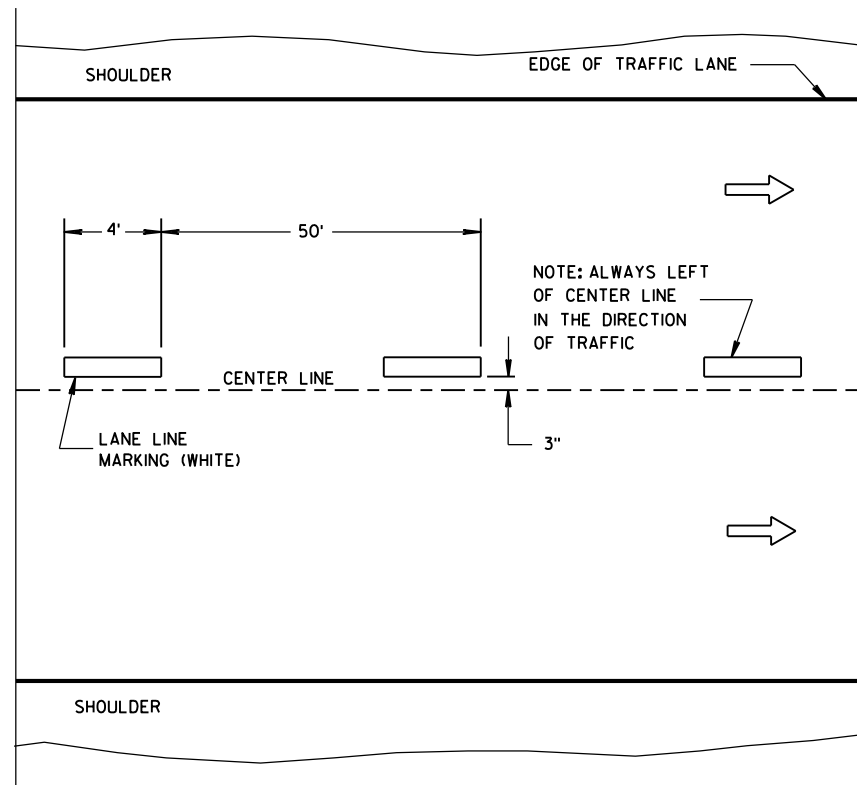


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

LEGEND

- "T" MARKING
- POST MOUNTED SIGN

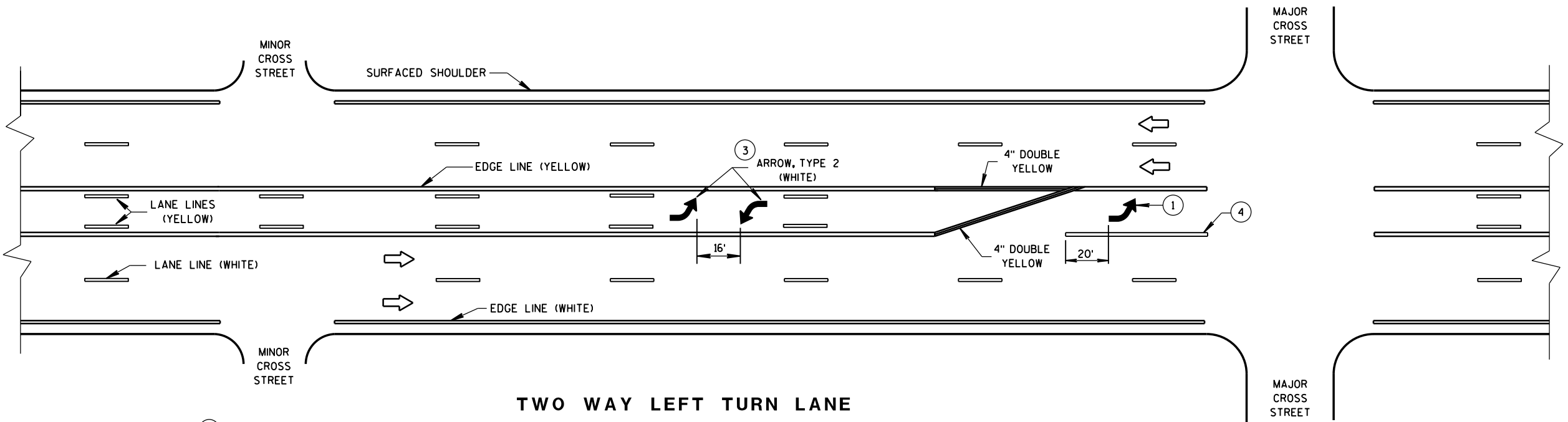
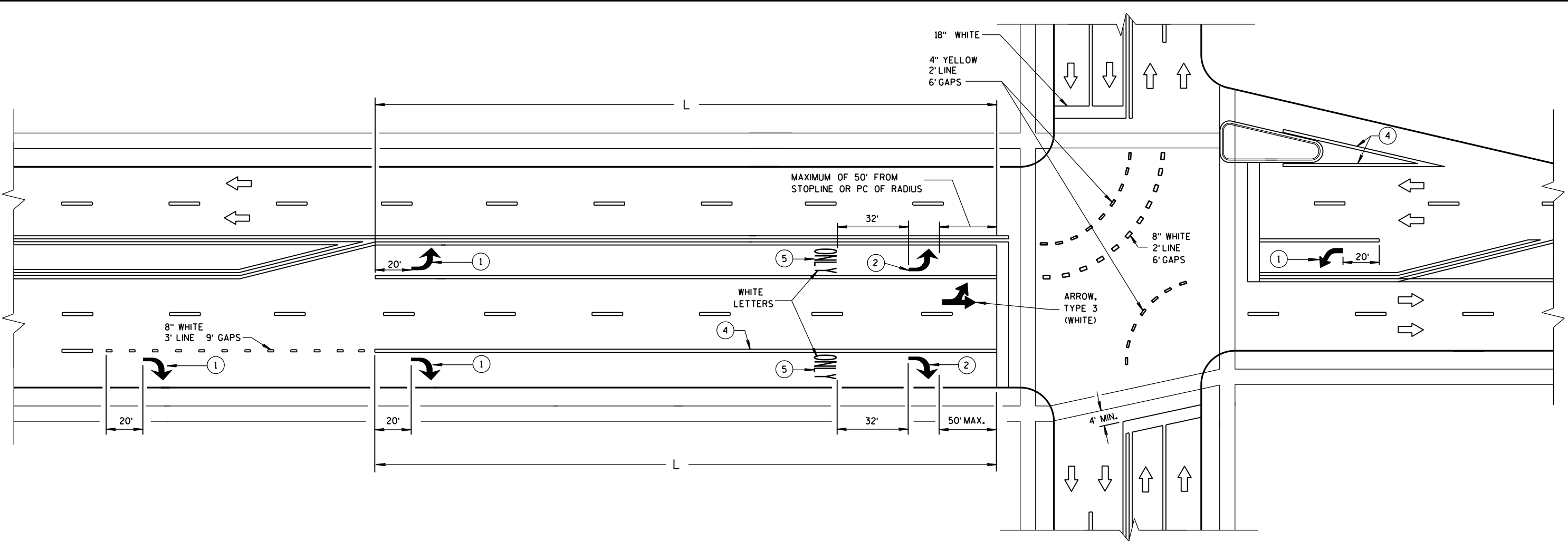
6

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S.D.D. 15 C 8-18a

S.D.D. 15 C 8-18a

LONGITUDINAL MARKING (MAINLINE)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017	/S/ Matthew R. Rauch
DATE	STATE SIGNING AND MARKING ENGINEER
FHWA	



TWO WAY LEFT TURN LANE

GENERAL NOTES

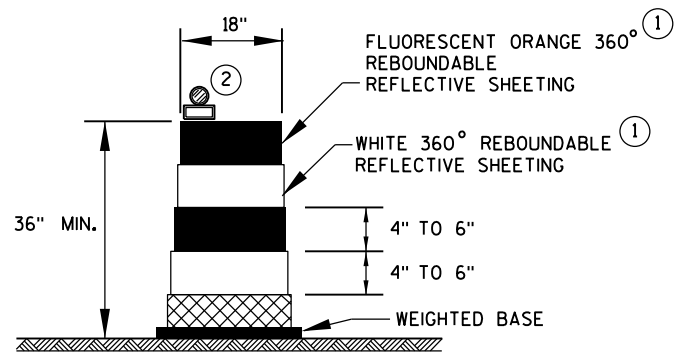
- ① REQUIRED ARROW, TYPE 2 (WHITE).
- ② REQUIRED ARROW, TYPE 2 (WHITE) WHEN L IS GREATER THAN 78 FEET AND LESS THAN OR EQUAL TO 166 FEET.
- ③ A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ④ 8" WHITE
- ⑤ REQUIRED WORD ONLY WHEN L IS GREATER THAN 166 FEET.

NOTE:
 ARROW SYMBOL ()
 SHOWS DIRECTION OF TRAVEL

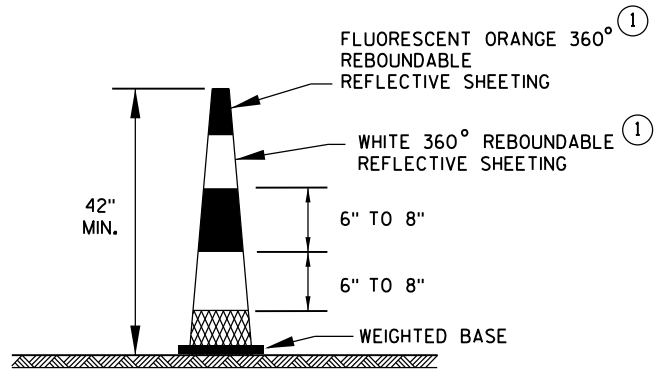
L = LENGTH OF TURN BAY

**PAVEMENT MARKING
(TURN LANES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



DRUM

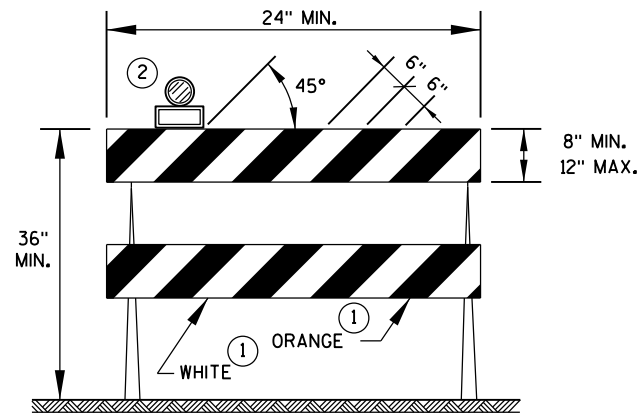


42" CONE

DO NOT USE IN TAPERS
 1/2 SPACING OF DRUMS

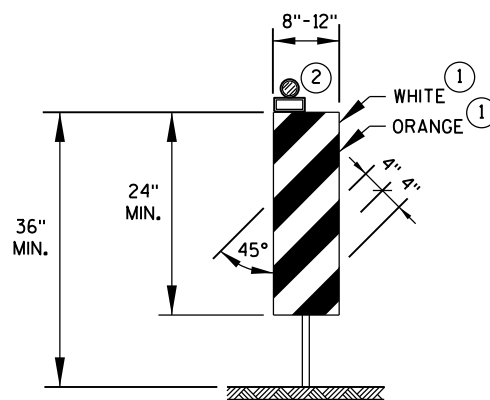
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



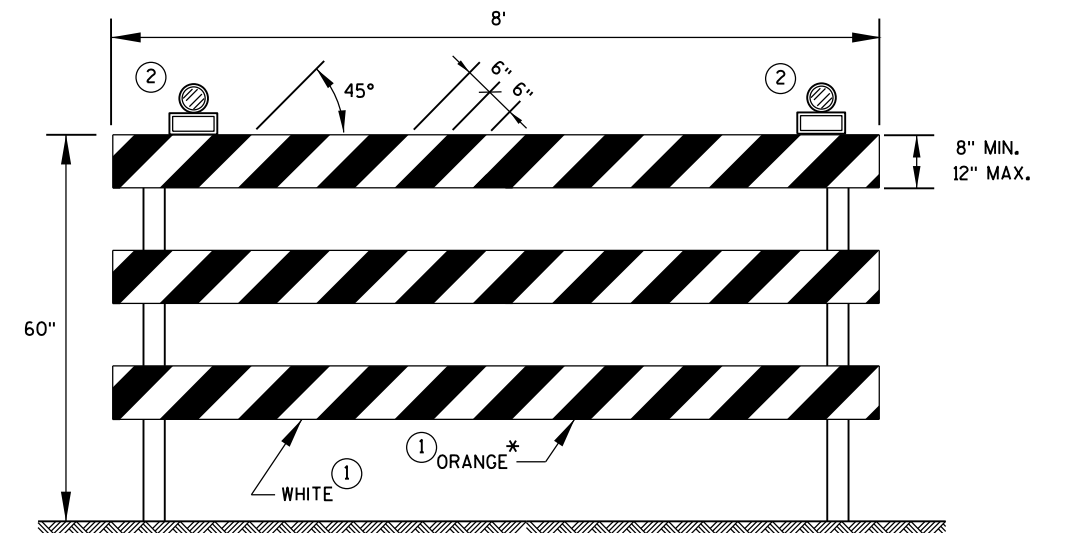
TYPE 2 BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
 ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE 3 BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.





* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

6

6

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/s/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

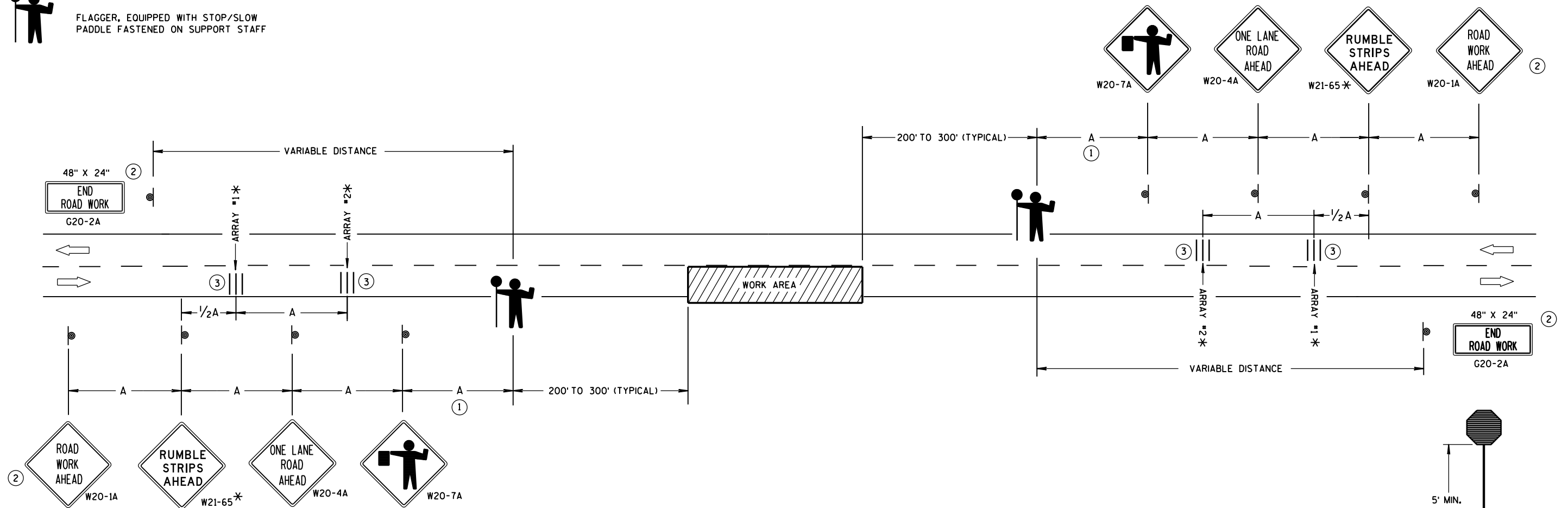
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING A
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING A.



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"W0" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS. PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* UTILIZE TEMPORARY RUMBLE STRIPS WHEN FLAGGING OPERATION IS ANTICIPATED TO BE STATIONARY IN EXCESS OF TWO HOURS.

- ① FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS (IF USED) SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ③ EACH TEMPORARY RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2017 /S/ Andrew Heldtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.



ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.


THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.


WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
 OR 
W21-64 W21-64
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

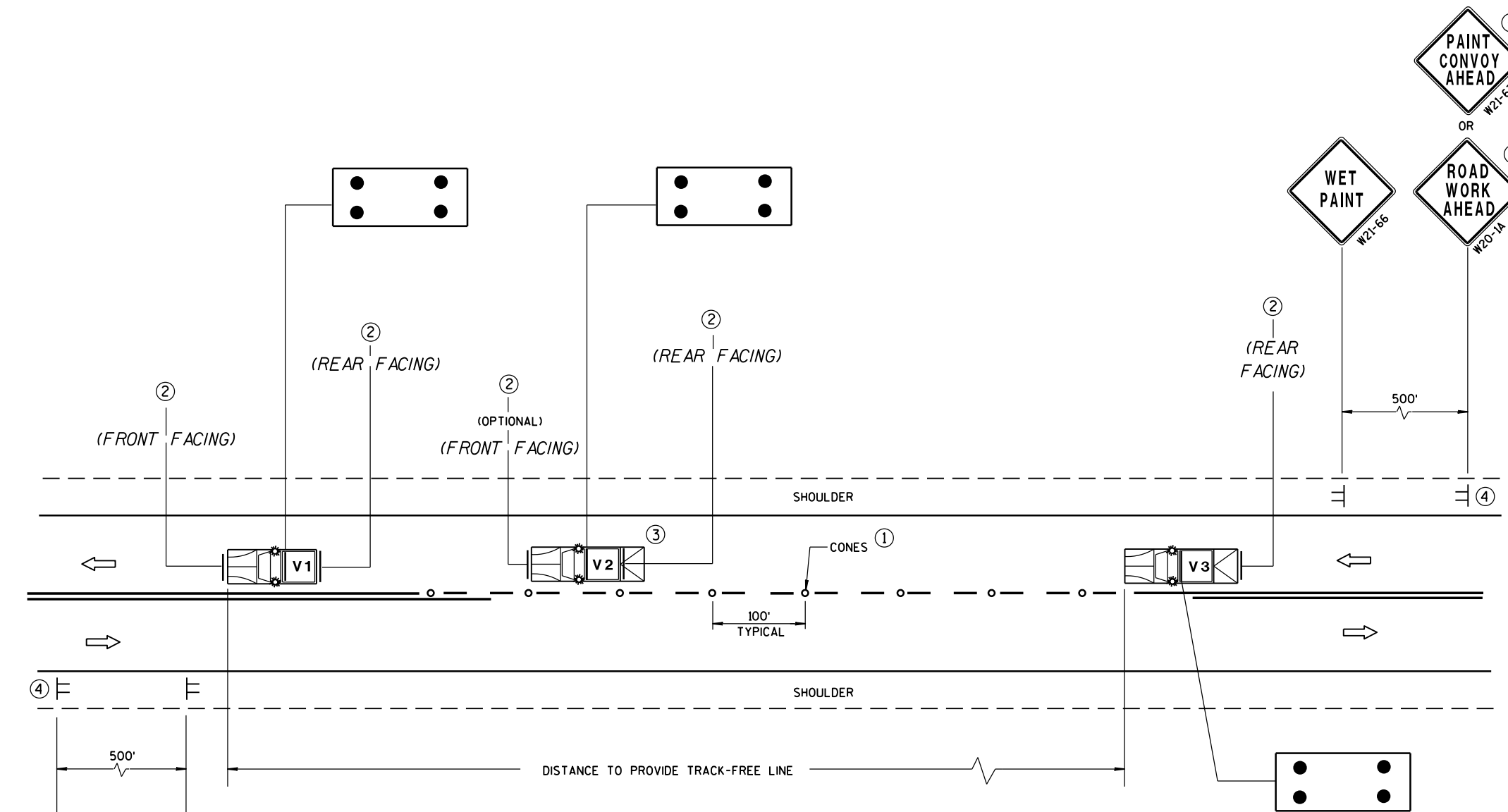
- V1** LEAD VEHICLE
- V2** SHADOW VEHICLE
- V3** TRAIL VEHICLE WITH TMA
-  **TMA** TRUCK-MOUNTED ATTENUATOR

 SIGN ON TEMPORARY SUPPORT

 DIRECTION OF TRAFFIC

 CONES

 FLASHING ARROW PANEL (CAUTION)



MOVING PAVEMENT MARKING OPERATIONS TWO-LANE TWO-WAY ROADWAY

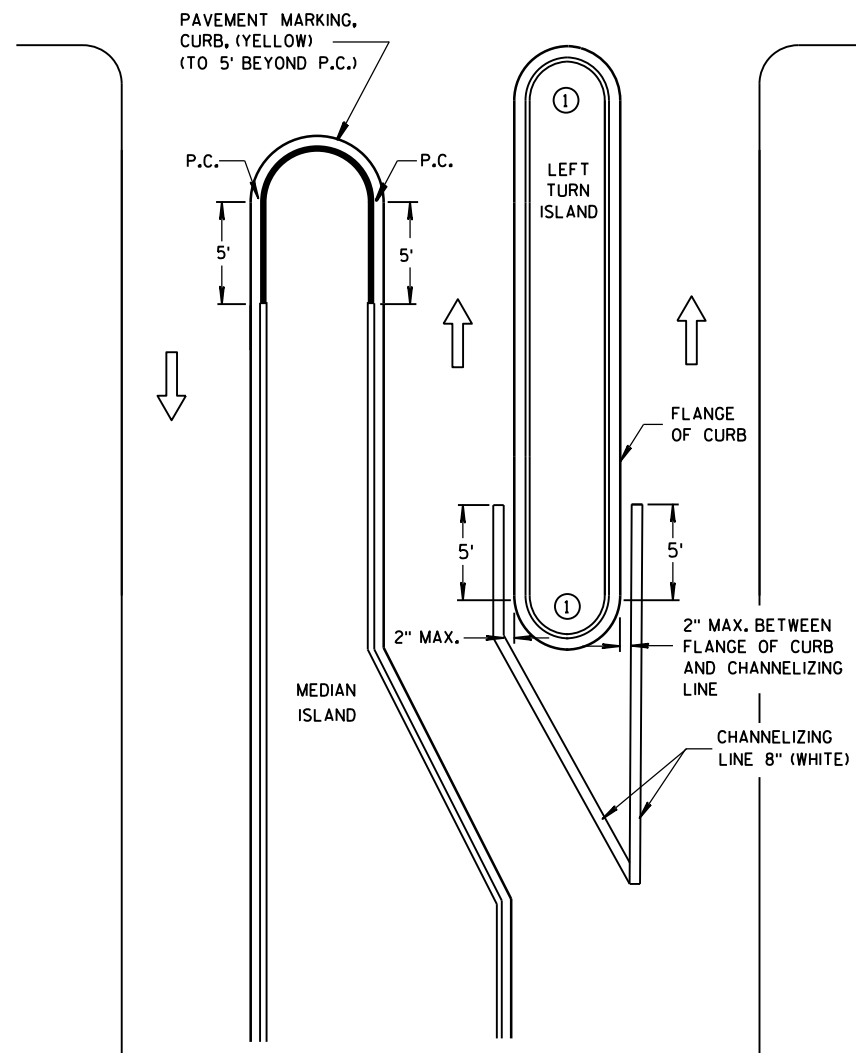
MOVING PAVEMENT MARKING
OPERATION
TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

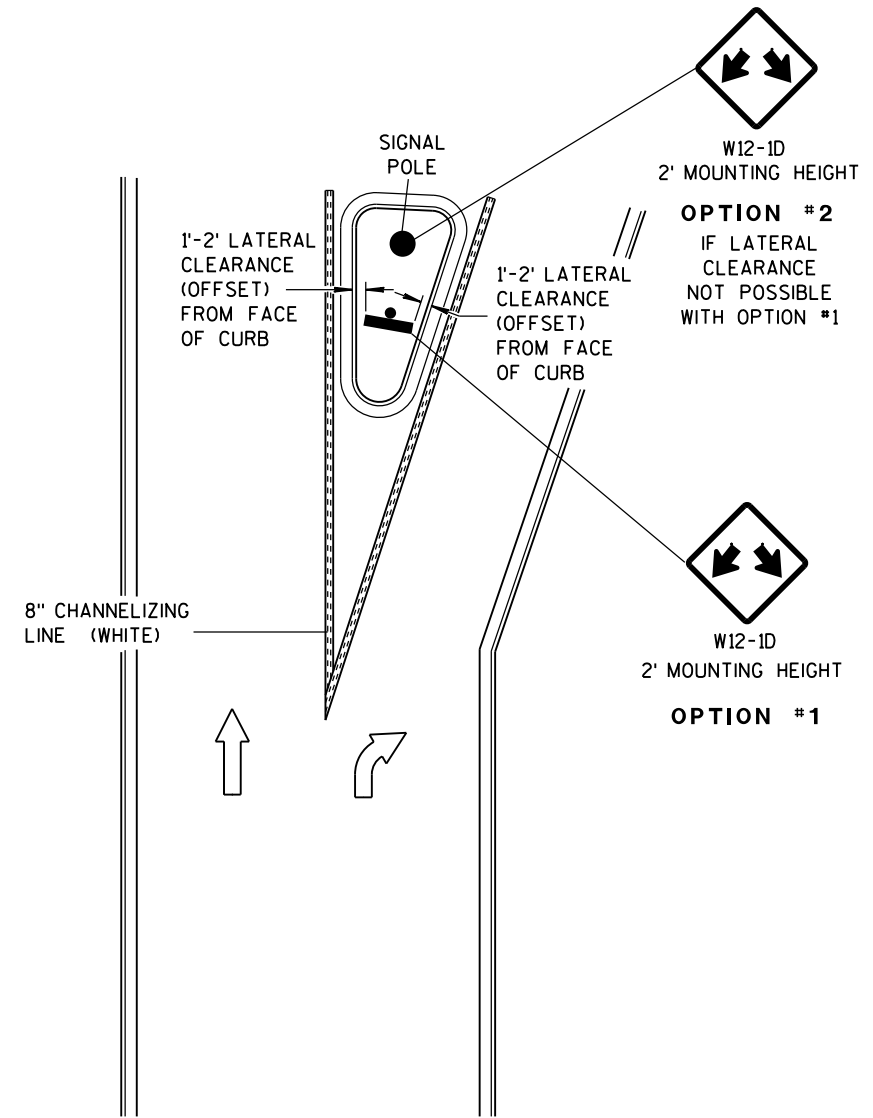
APPROVED
 Sept., 2017 /S/ Andrew Heidtke
 DATE WORK ZONE ENGINEER
 FHWA

GENERAL NOTE

APPLIES TO ISLANDS AT LEFT TURNS AT ONE WAY ROADWAYS AS WELL.
SEE MISCELLANEOUS QUANTITIES FOR SIGN SIZE.



LEFT TURN & MEDIAN ISLAND

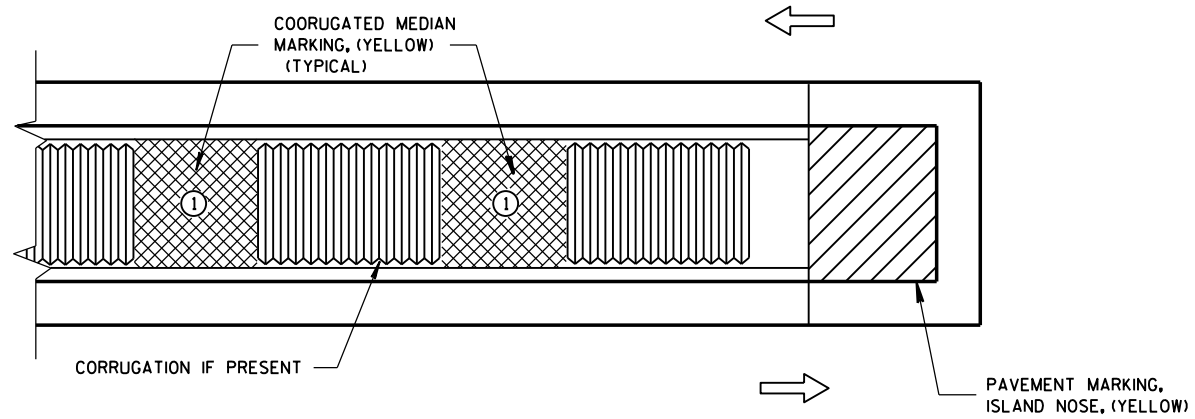


RIGHT TURN ISLAND

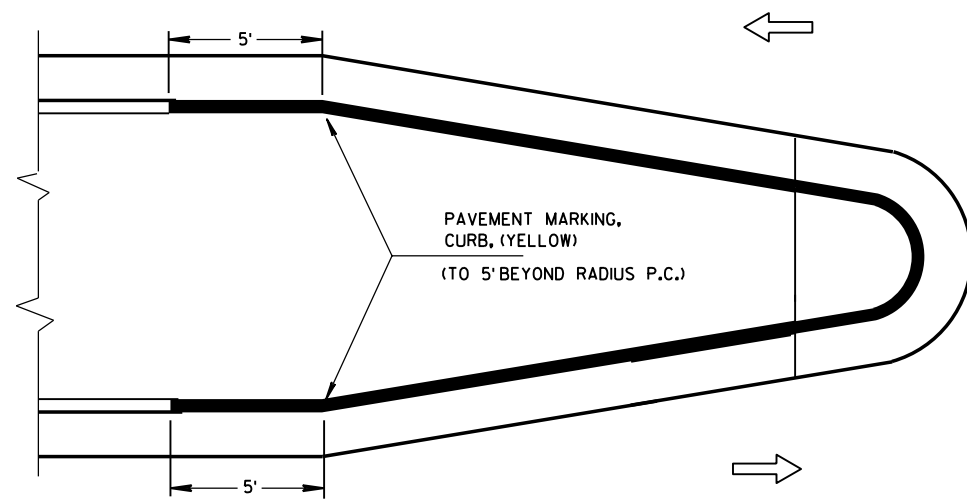
DOUBLE ARROW WARNING SIGN PLACEMENT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

6

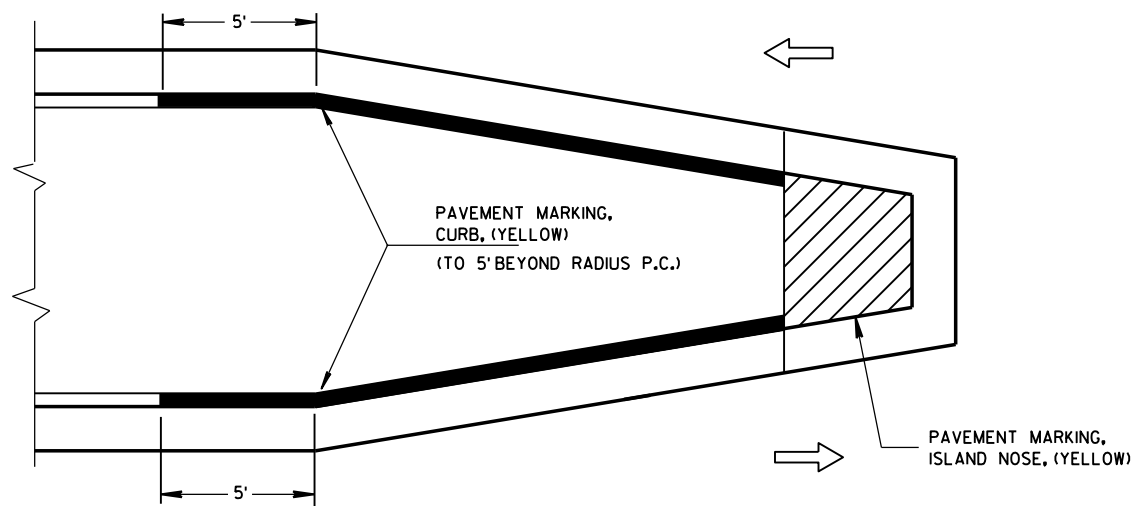
6



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE







MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

① WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

-  ISLAND NOSE MARKING
-  CURB MARKING
-  CORRUGATED MEDIAN MARKING
-  DIRECTION OF TRAVEL

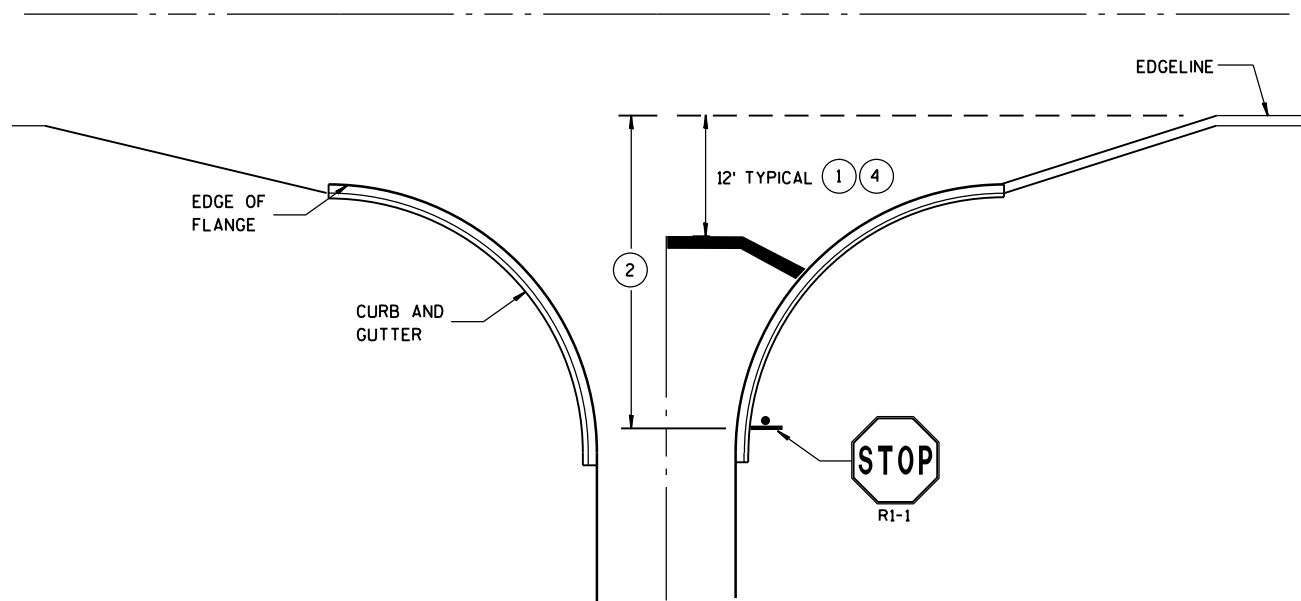
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6

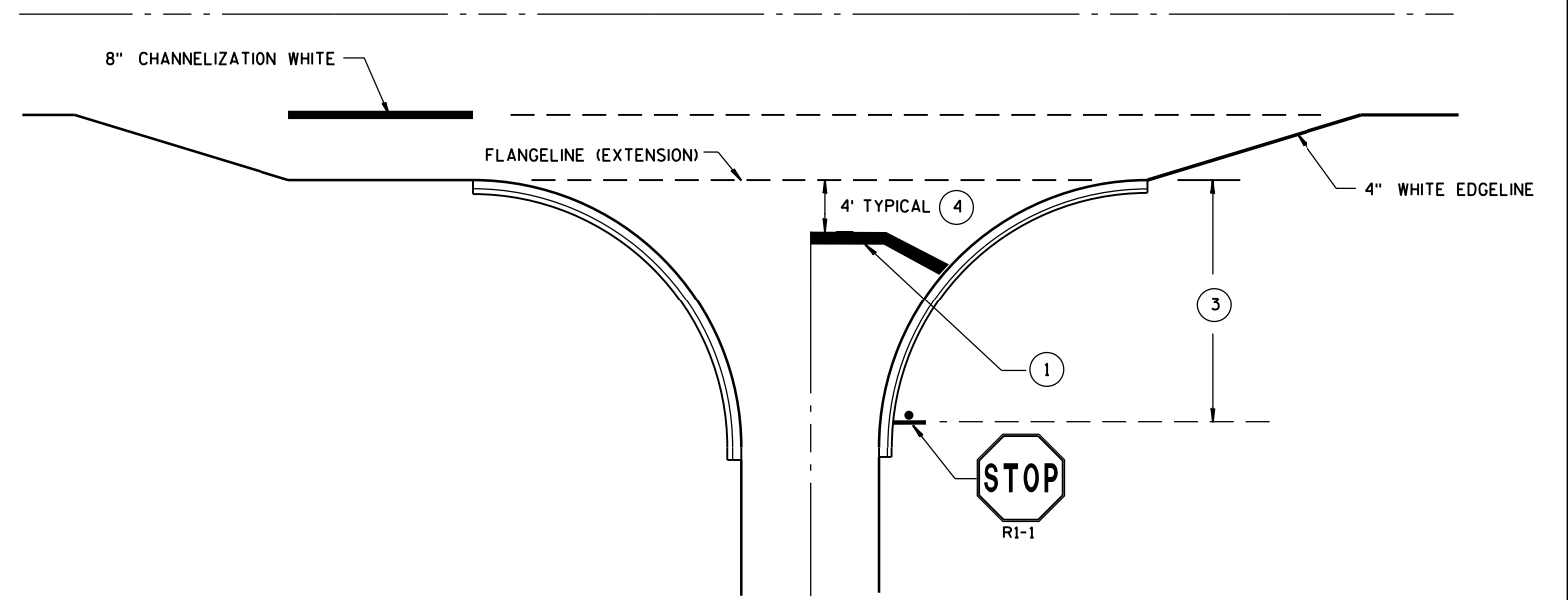
S.D.D. 15 C 27-2b

S.D.D. 15 C 27-2b

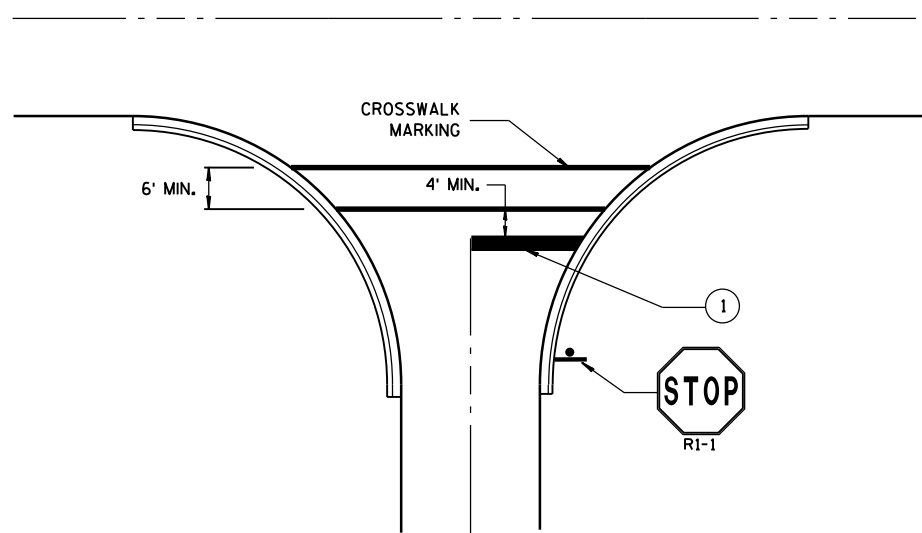
PAVEMENT MARKING (ISLANDS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	



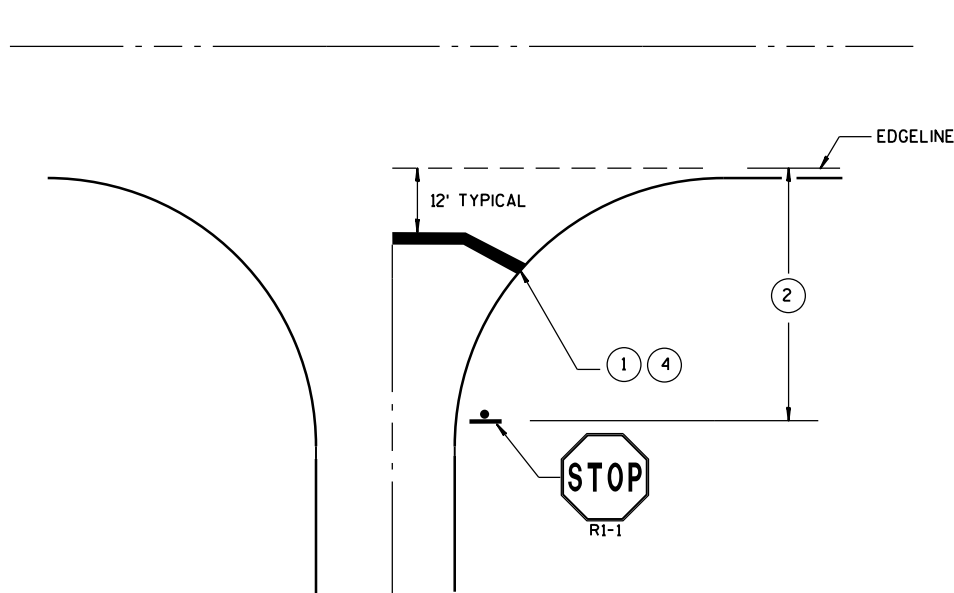
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

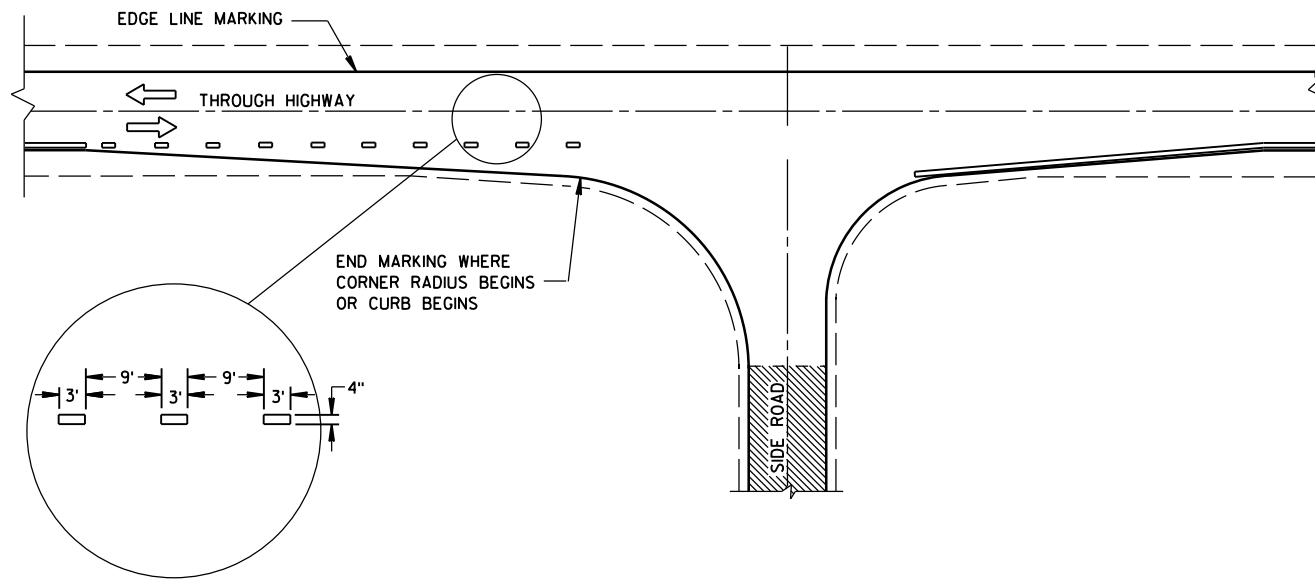
STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: Sept., 2017 /S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER
FHWA

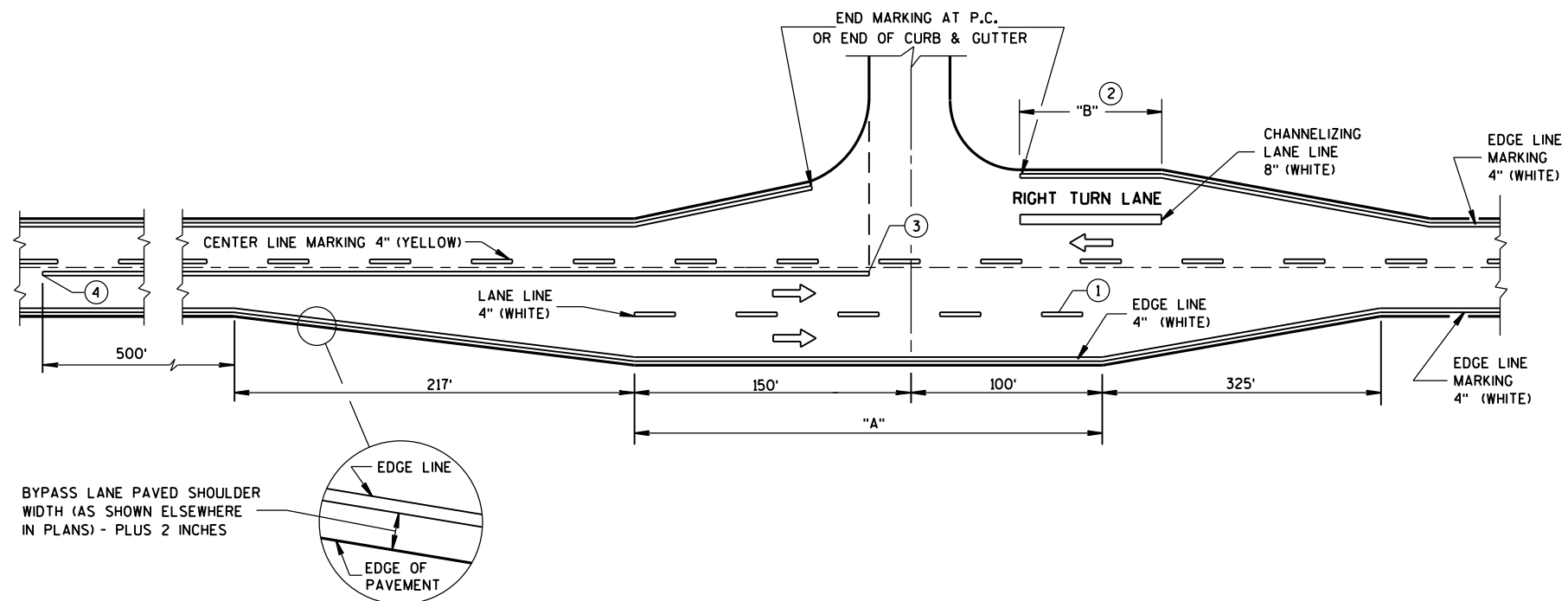


MINOR INTERSECTION

GENERAL NOTES

- OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
 - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
 - ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
 - ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

ARROW SYMBOL (\Rightarrow) SHOWS DIRECTION OF TRAVEL



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

TABLE A

SHOULDER TAPER LENGTH (FEET)		SHOULDER TAPER LENGTH (FEET)				BUFFER SPACE (FEET)
S	W	4	6	8	10	
30	20	30	40	50	200	
35	30	45	55	70	250	
40	40	55	75	90	305	
45	60	90	120	150	360	
50	70	100	135	170	425	
55	75	110	150	185	495	

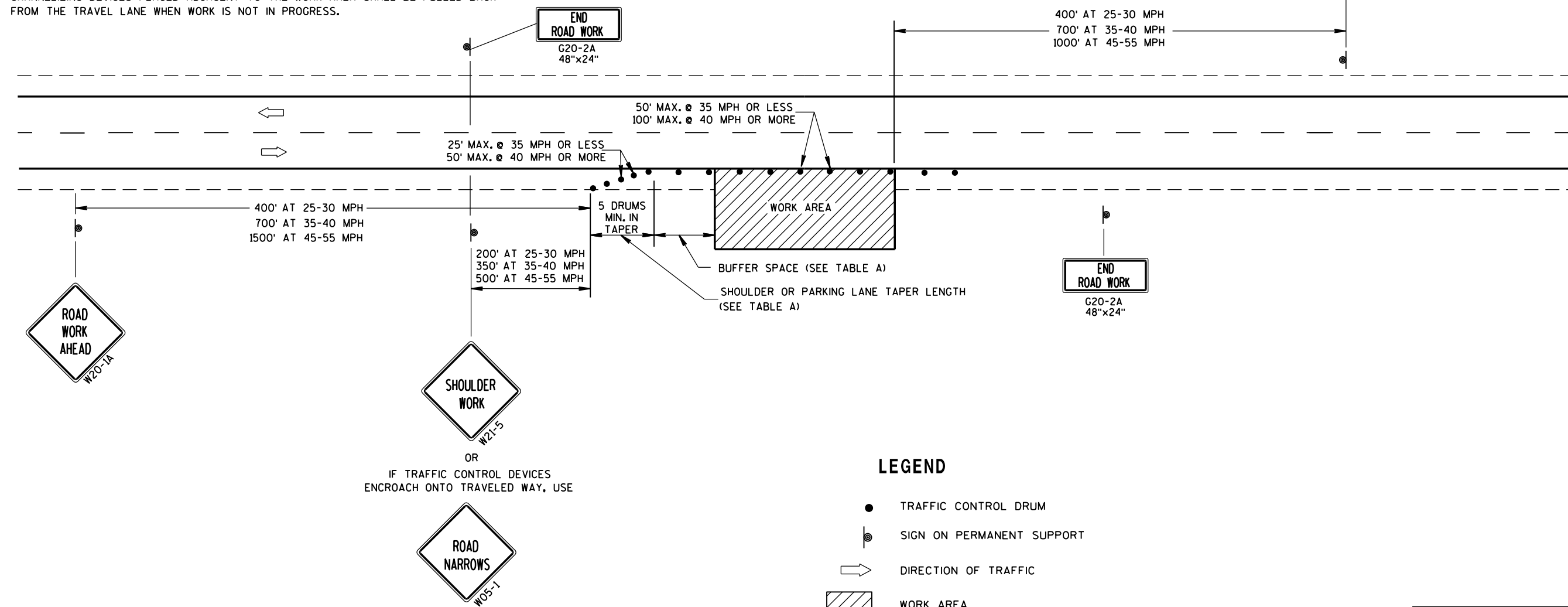
SHOULDER TAPER LENGTH = $\frac{1}{3}L$

W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH

L = WS AT 45 MPH OR GREATER

L = $\frac{WS^2}{60}$ AT 40 MPH OR LESS

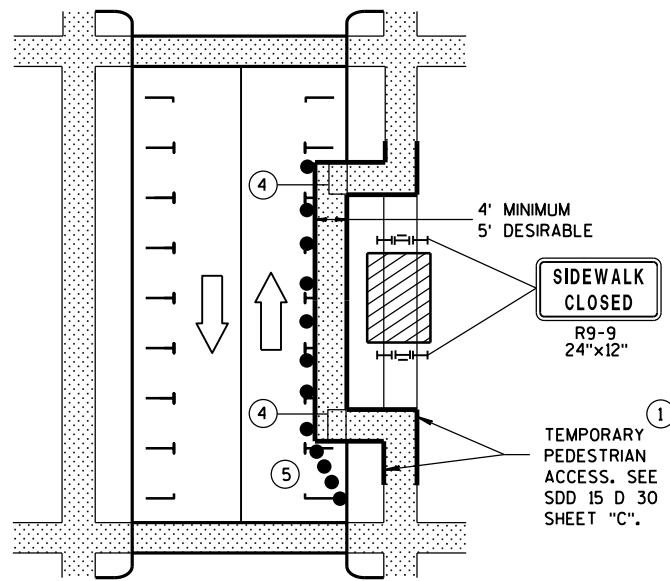


LEGEND

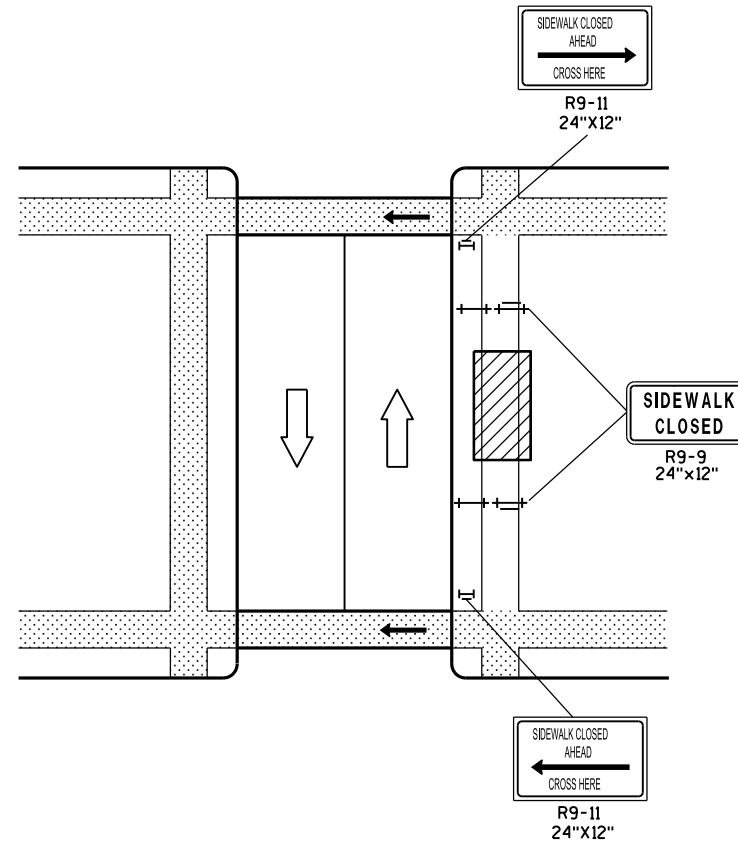
- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ➔ DIRECTION OF TRAFFIC
- ▨ WORK AREA

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 14, 2015 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

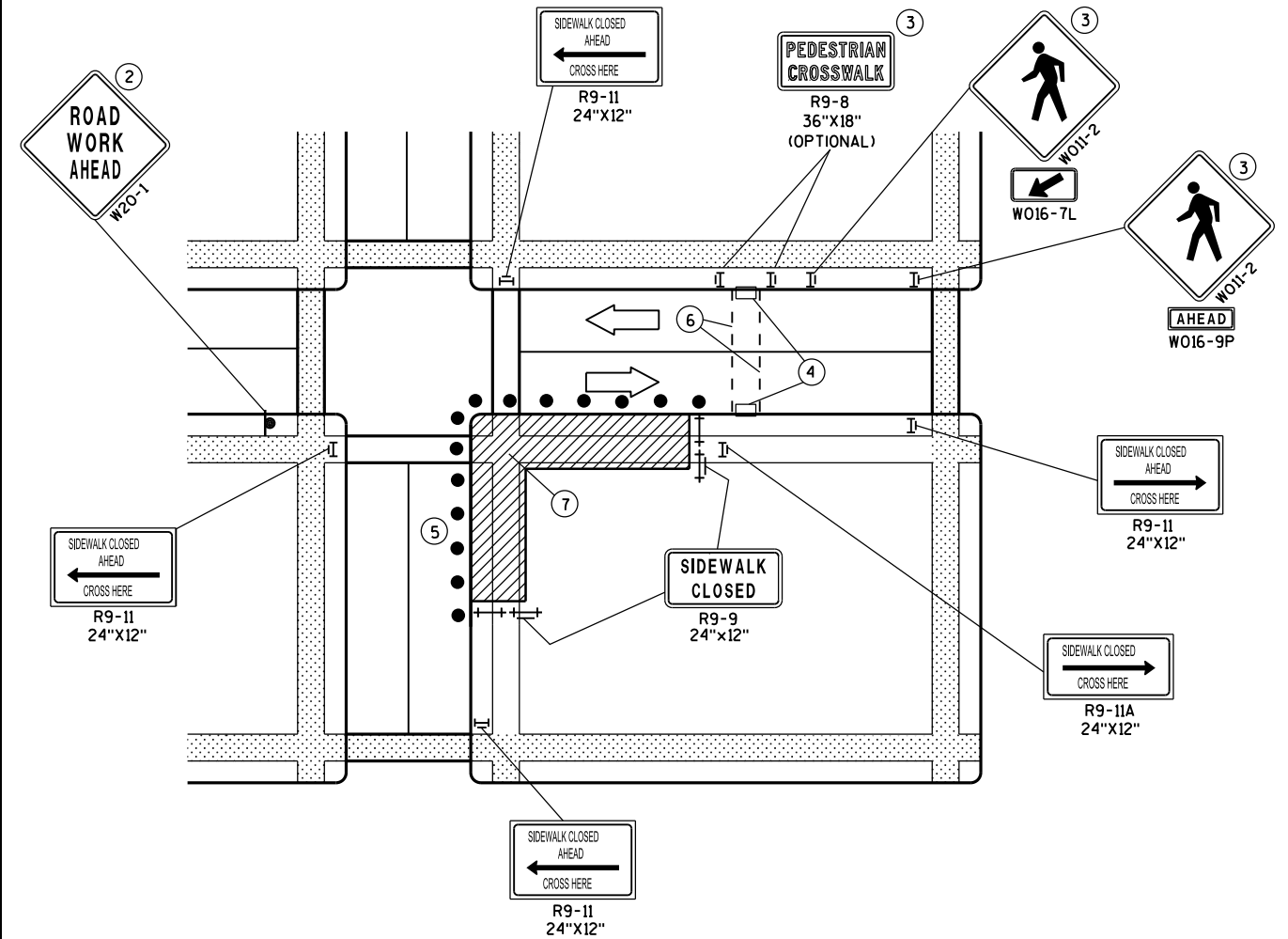
NOTE: MAY BE USED ON ROADWAY WITH POSTED SPEED OF LESS THAN 40 MPH.



MID-BLOCK SIDEWALK CLOSURE IN PARKING LANE

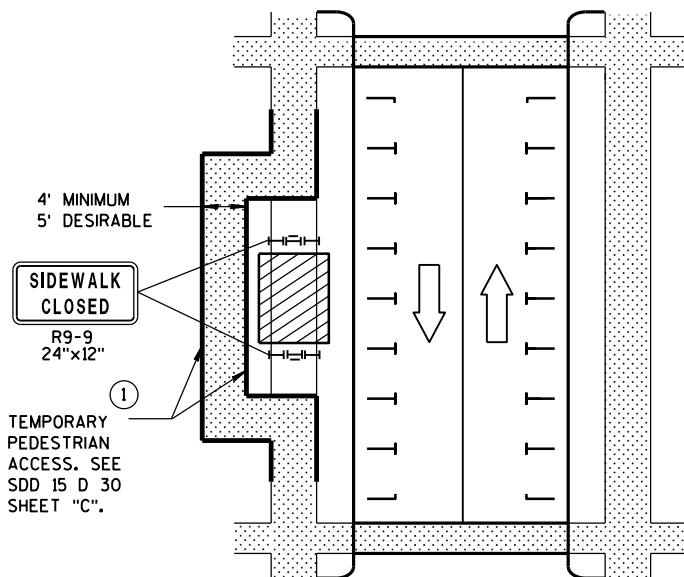


MID-BLOCK SIDEWALK CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

NOTE: LAYOUT SAME AS ABOVE.



SIDEWALK DIVERSION

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"W0" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- ① IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.
- ② "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- ③ IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND W011-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.
- ④ TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".
- ⑤ DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- ⑥ TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- ⑦ LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

LEGEND

	SIGN ON PERMANENT SUPPORT		DIRECTION OF TRAFFIC
	UNDER PEDESTRIAN TRAFFIC		TRAFFIC CONTROL DRUM
	WORK AREA		
	PEDESTRIAN CHANNELIZATION DEVICE		
	TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		
	TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		

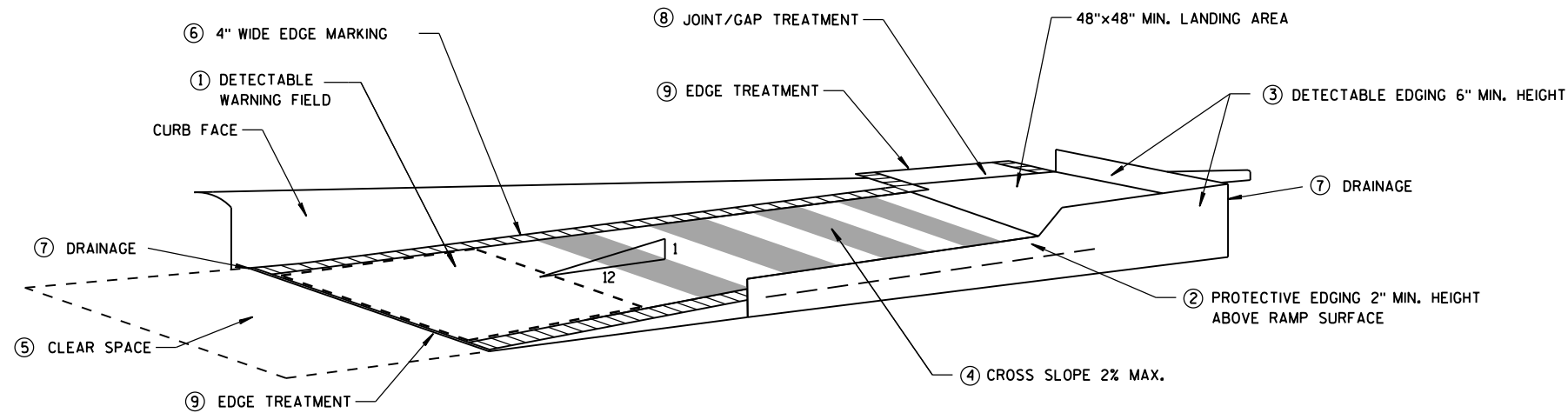
**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

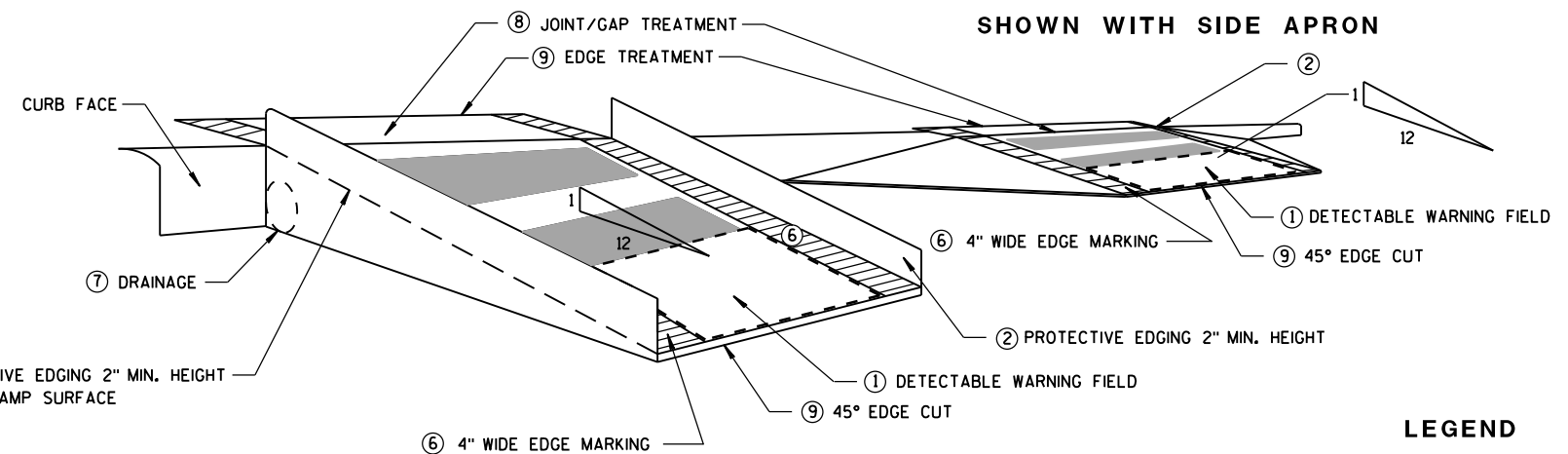
GENERAL NOTES

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- ① CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 8D5 SHEET "E".
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
- ⑤ CLEAR SPACE OF 48"x48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
- ⑥ THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
- ⑦ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑧ LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- ⑨ CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- ⑩ 5' WIDE MIN. WITH PEDESTRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.

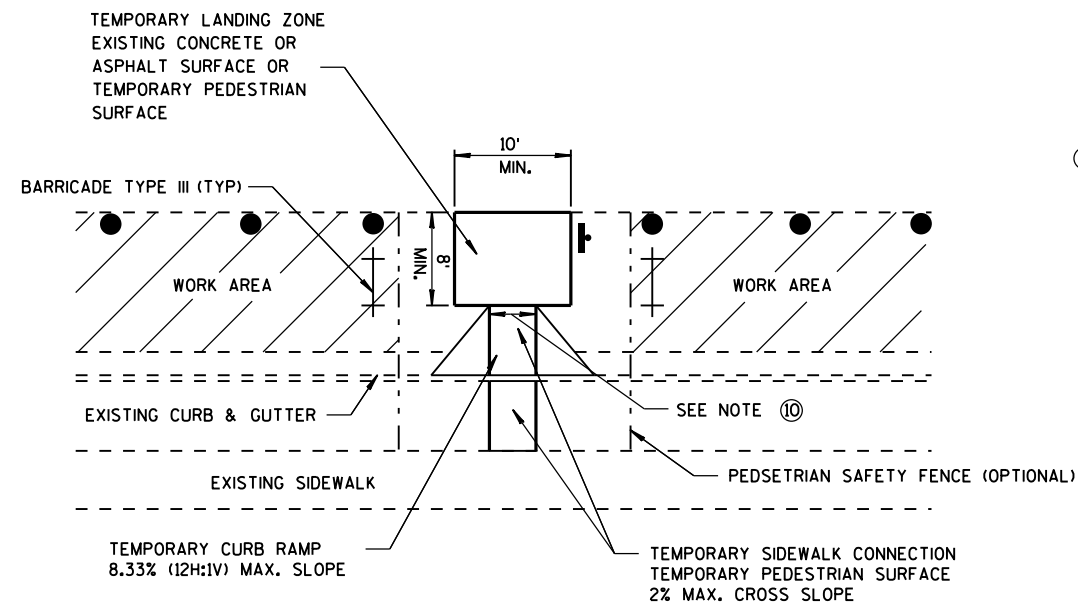


**TEMPORARY CURB RAMP
PARALLEL TO CURB**



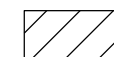


SHOWN WITH PROTECTIVE EDGE

**TEMPORARY CURB RAMP
PERPENDICULAR TO CURB**



TEMPORARY BUS STOP PAD

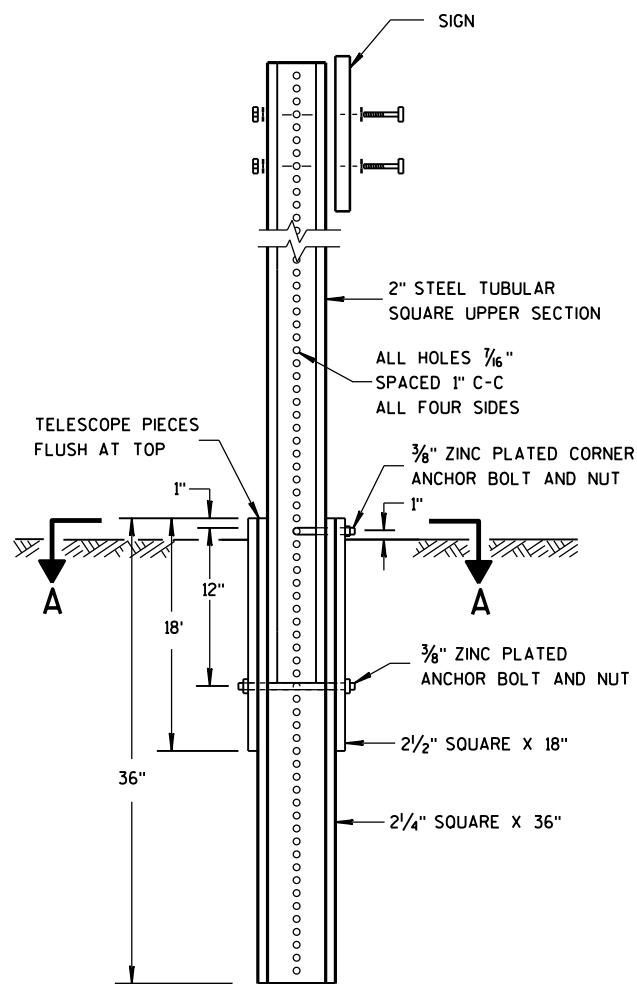
LEGEND

-  WORK AREA
-  TYPE III BARRICADE
-  TRAFFIC CONTROL DRUM

**TRAFFIC CONTROL,
TEMPORARY ADA COMPLIANT
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



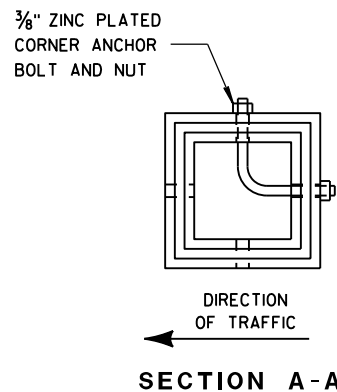
DETAIL OF TUBULAR STEEL SIGN POST

TUBULAR STEEL POSTS

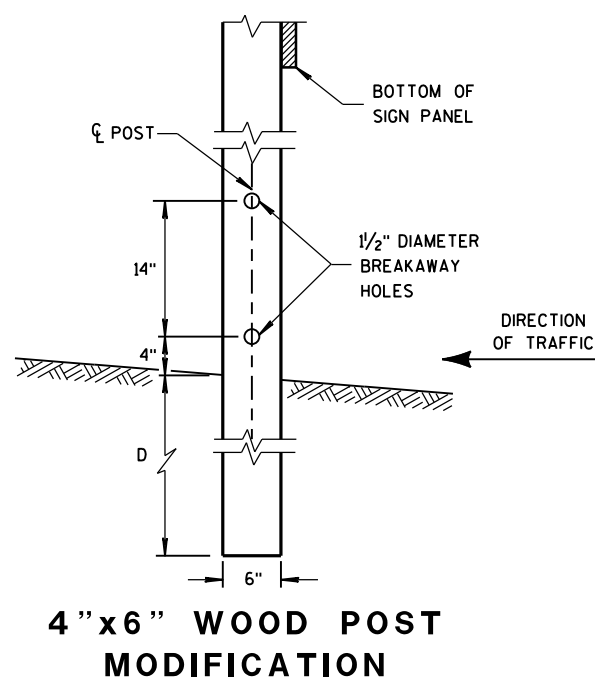
AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

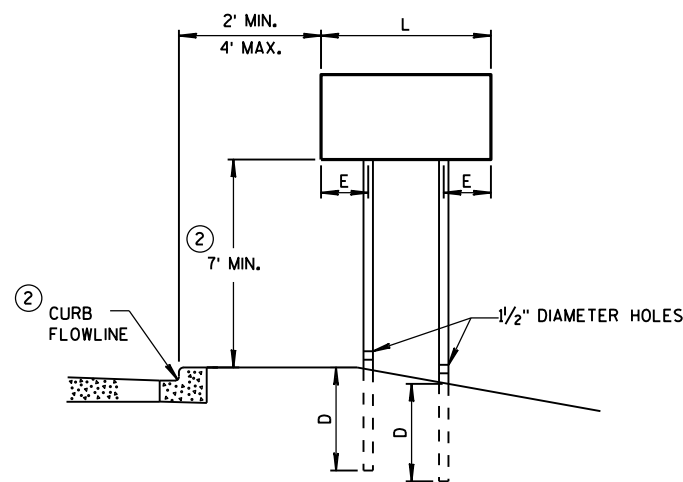
SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.



SECTION A-A



4" X 6" WOOD POST MODIFICATION

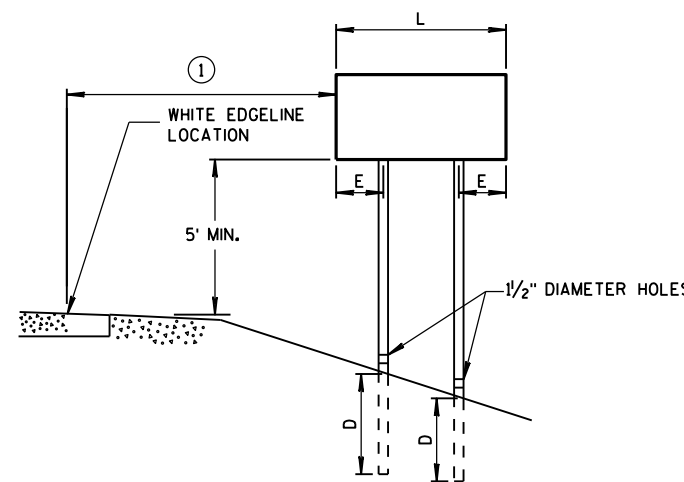


URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



RURAL AREA

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

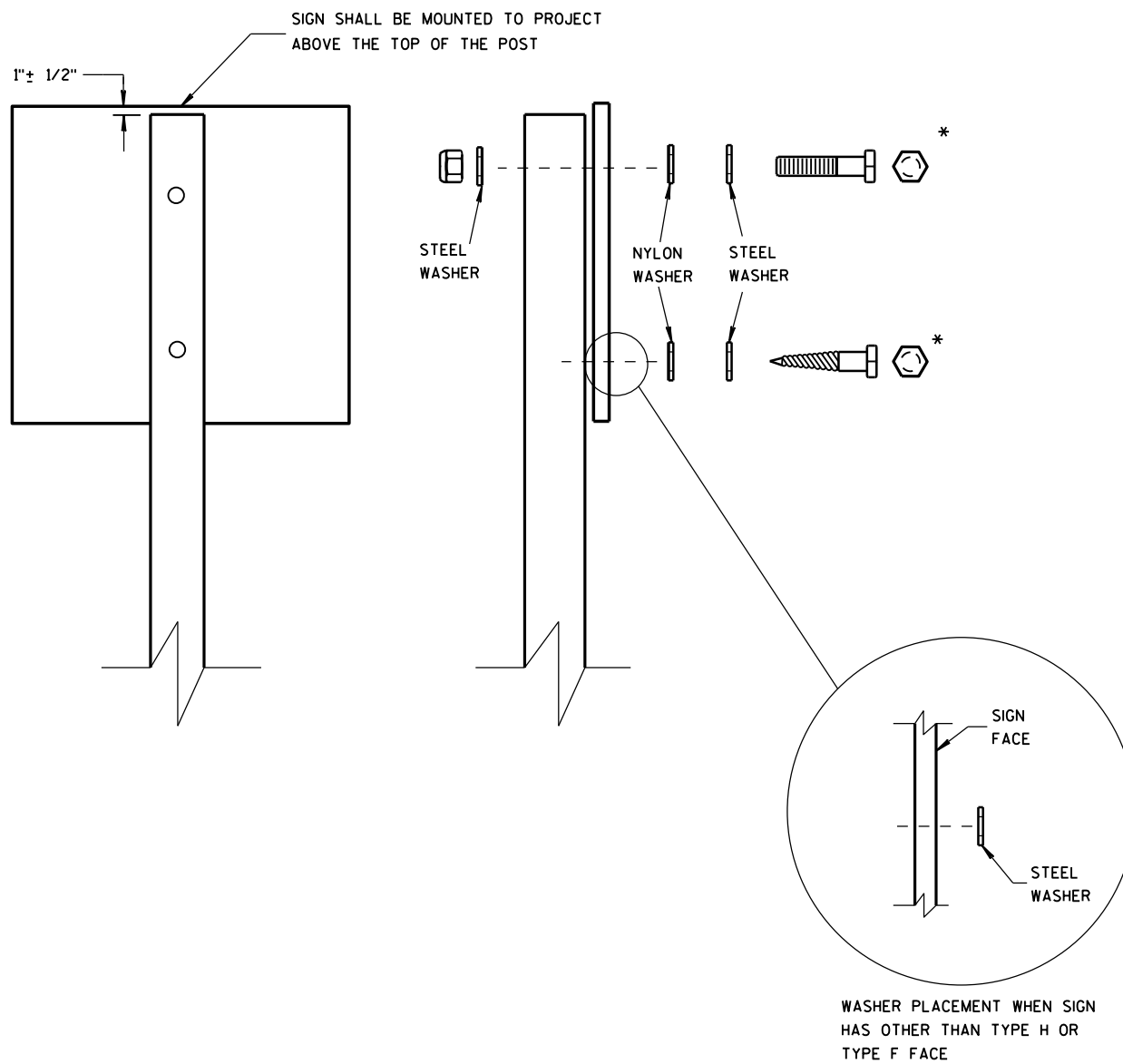
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3

B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" x 3"

MACHINE BOLTS - 5/16" x 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" x 3-1/4" LENGTH W/ NUTS

RIVETS - 5/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. x 3/8" I.D. x 1/16" STEEL

1-1/4" O.D. x 3/8" I.D. x .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017

DATE

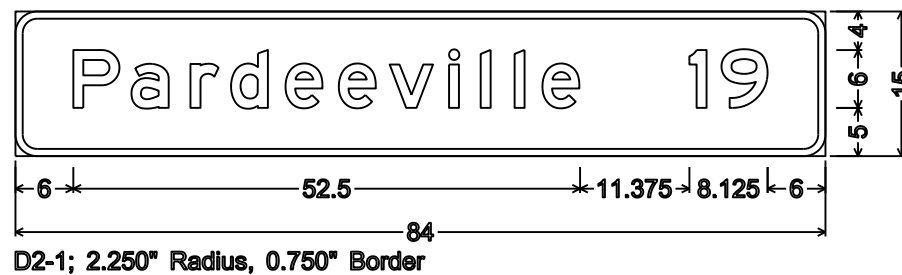
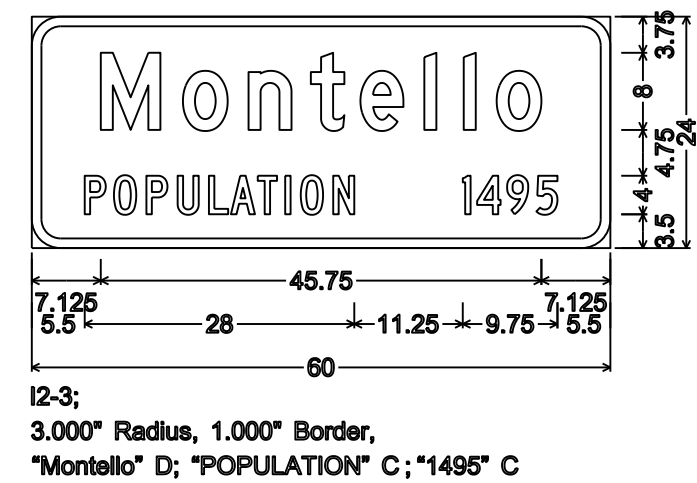
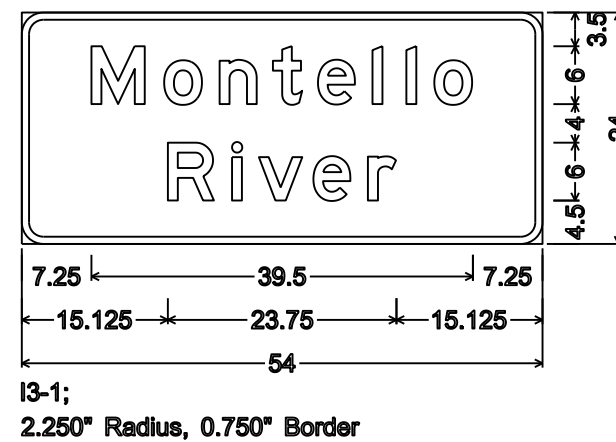
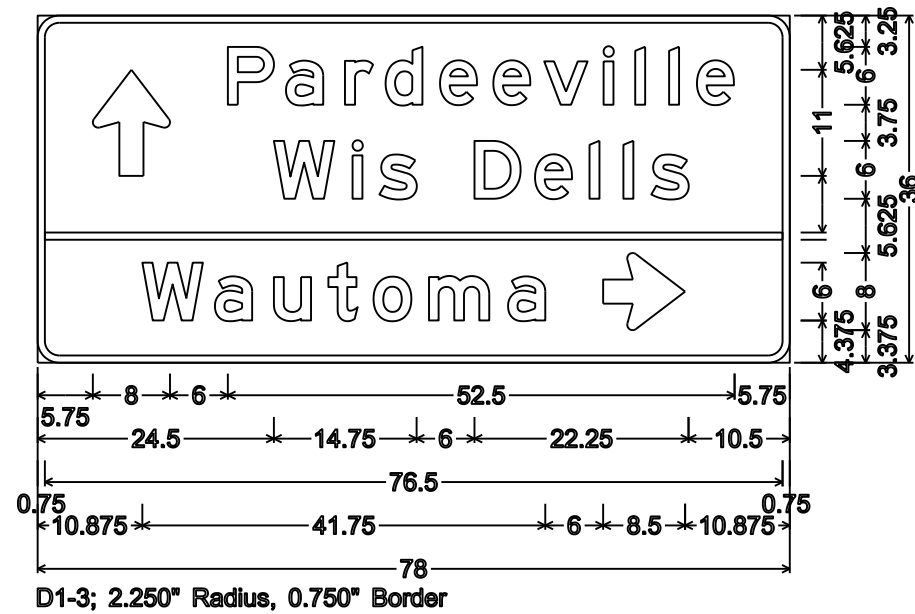
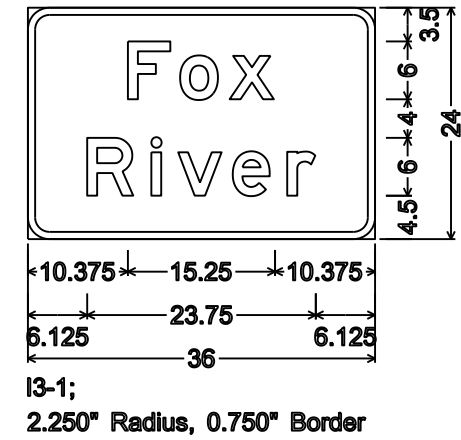
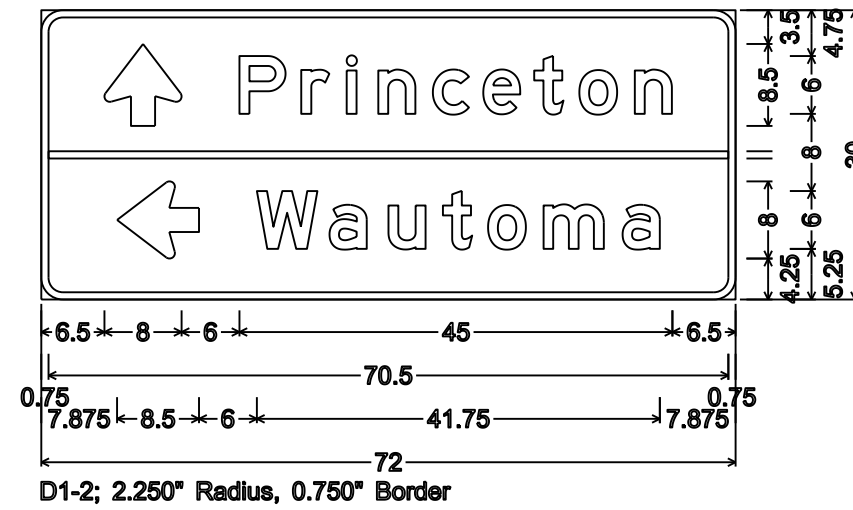
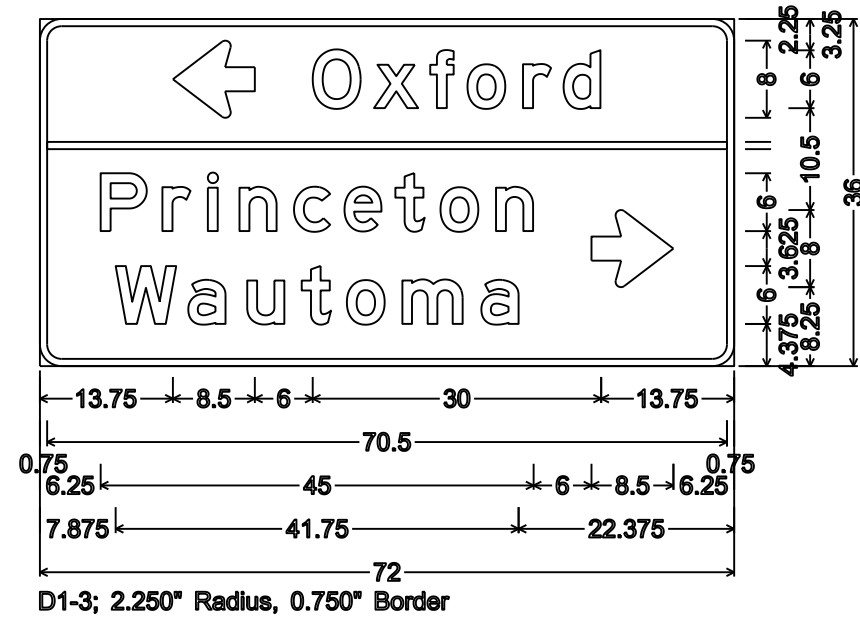
FHWA

/s/ Andrew Heidtke

WORK ZONE ENGINEER

NOTES

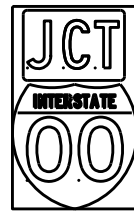
1. Signs are Type II- Type H Reflective
2. Color:
Background - Green
Message - White
3. Message Series - E except as noted



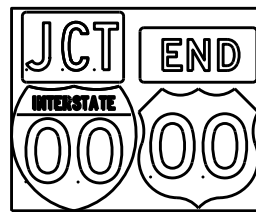
7

7

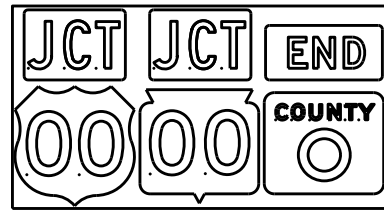
TYPICAL ASSEMBLIES



JI-1



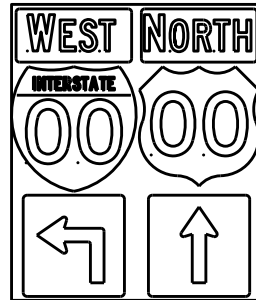
JI-2



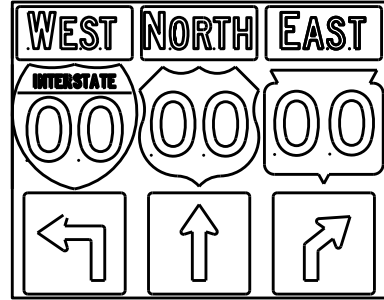
JI-3



J2-1



J2-2

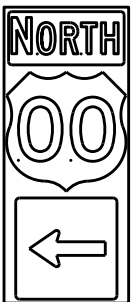


J2-3

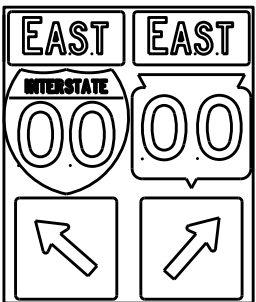


JV

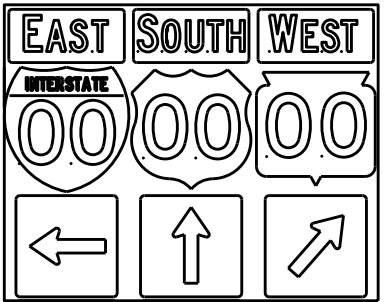
(Typical Vertical J-Assembly
See Note 10 and 11)



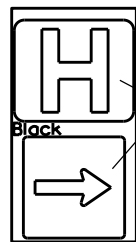
J3-1



J3-2

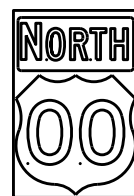


J3-3

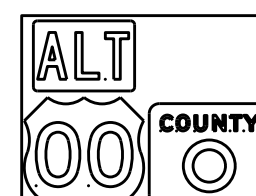


JH-1

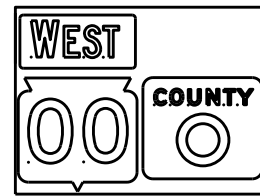
Blue Background



J4-1

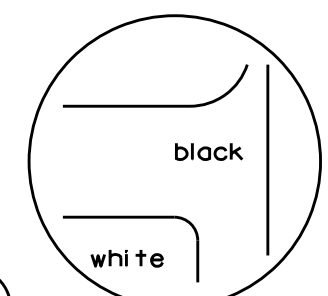
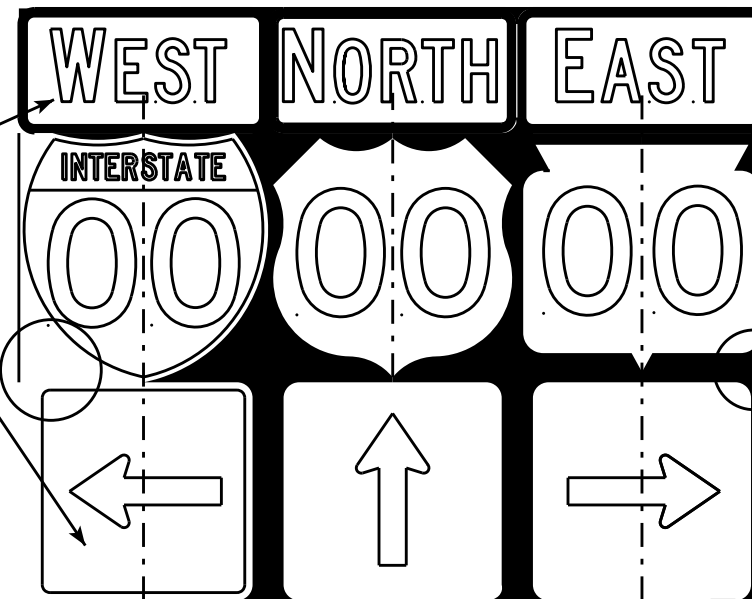
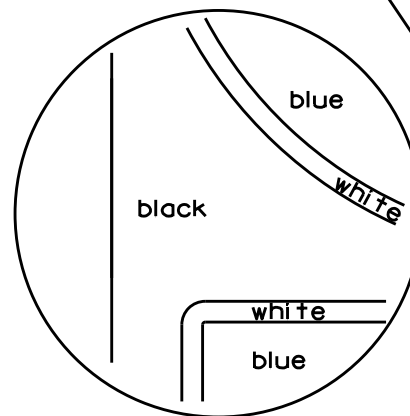


J4-2

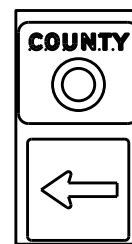


J4-2

[blue background with interstate]



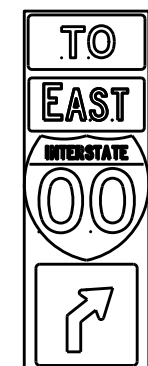
[black background]



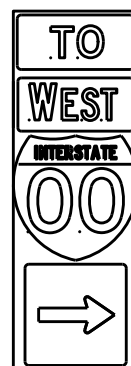
JI3-1



JI2-1



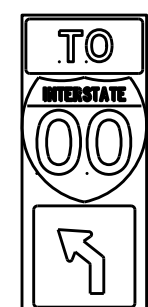
J32-1



J33-1



J23-1



J22-1

NOTES

- Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Black Non-reflective
Message - see Note 5
- Message Series - See Note 5
- Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an MI-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

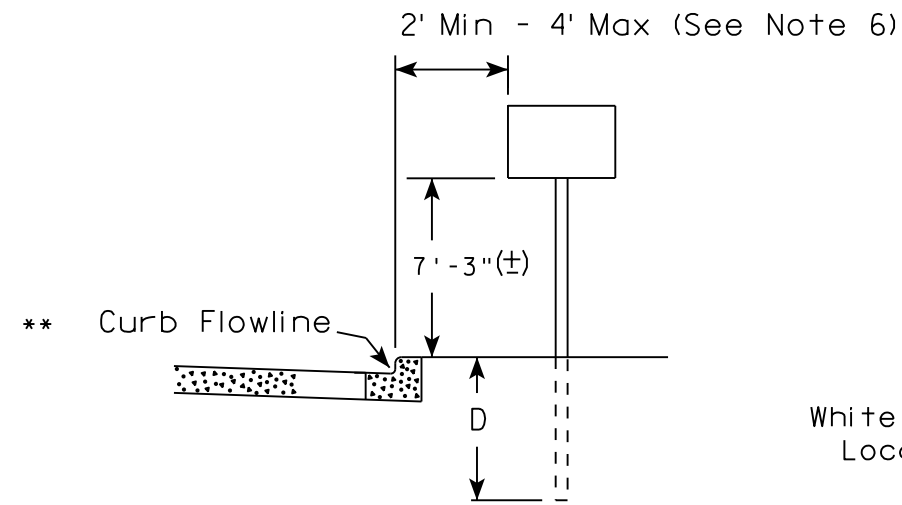
**ROUTE MARKERS & COMPONENTS
IN TYPICAL ASSEMBLIES**

WISCONSIN DEPT OF TRANSPORTATION

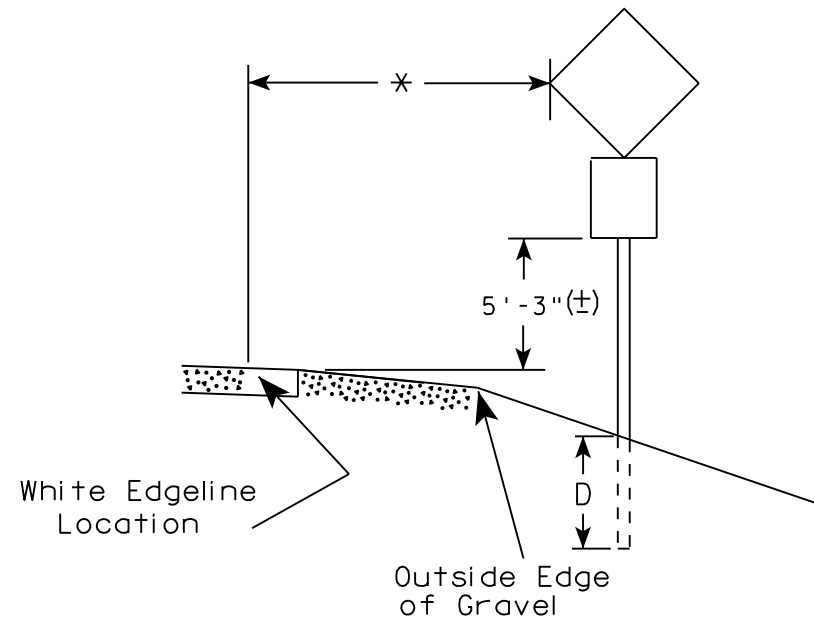
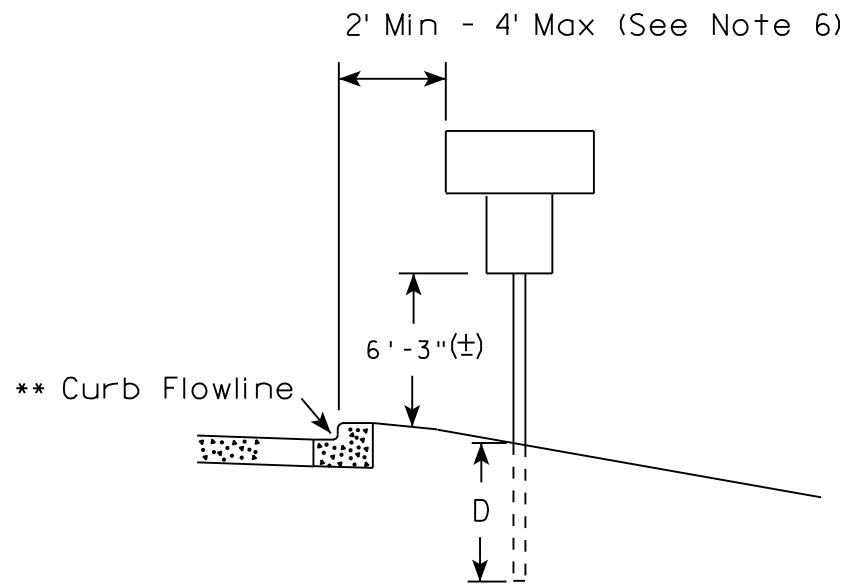
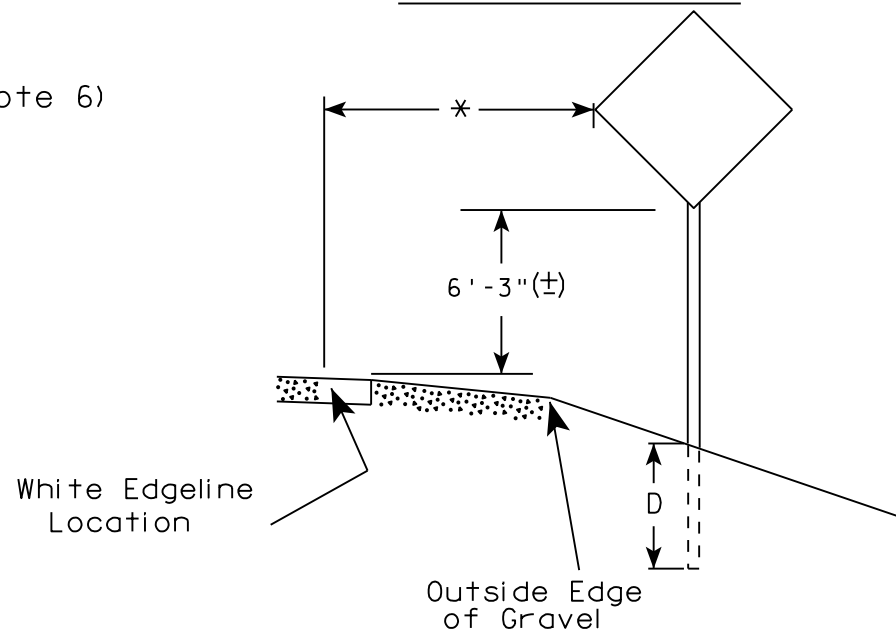
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/06/14 PLATE NO. A2-1S.8

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. J-Assemblies are considered to be one sign for mounting height.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-3.21



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

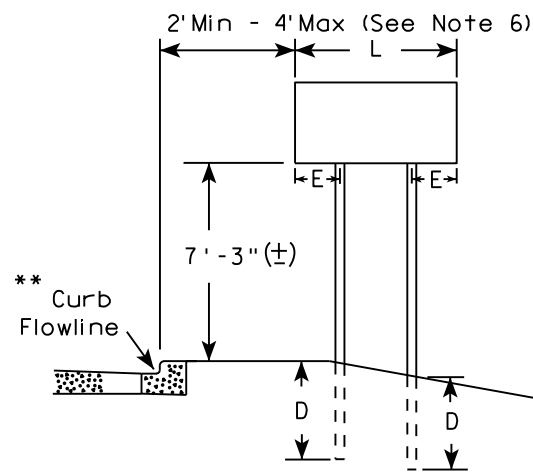
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

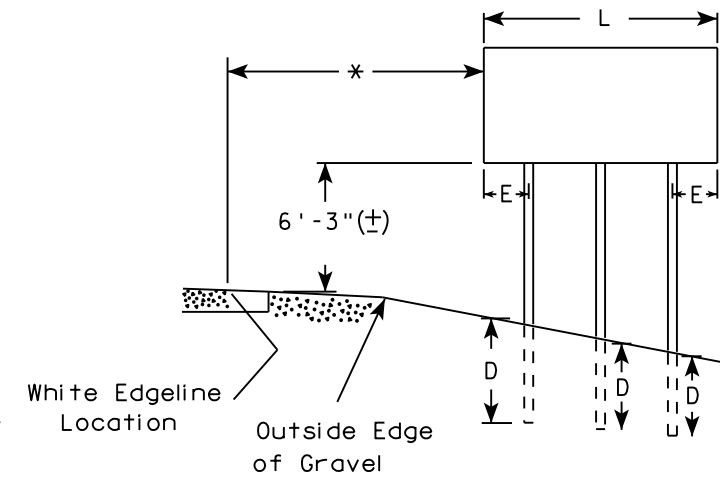
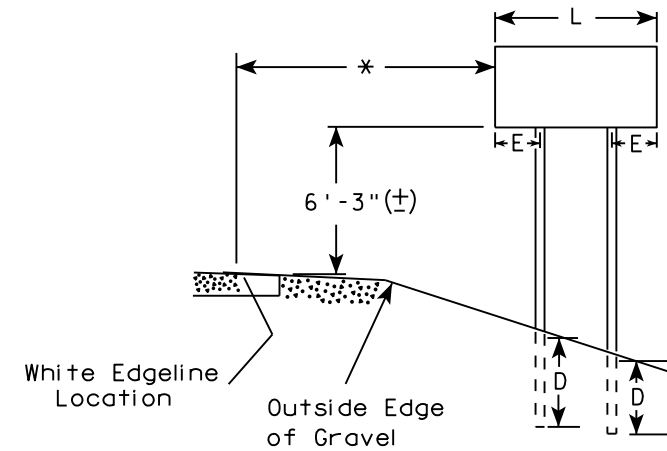
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

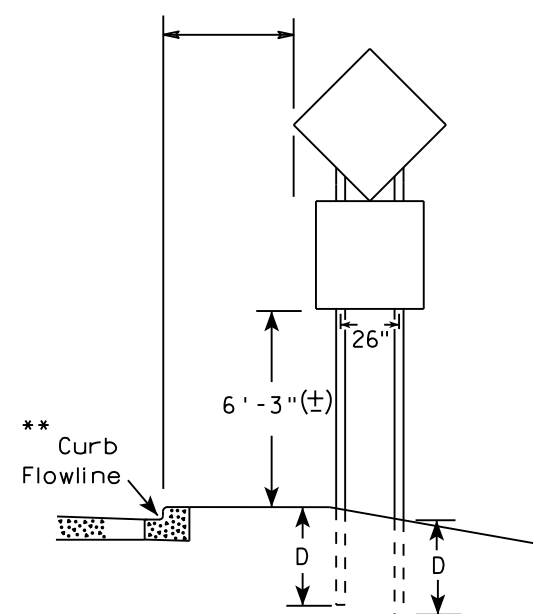
URBAN AREA



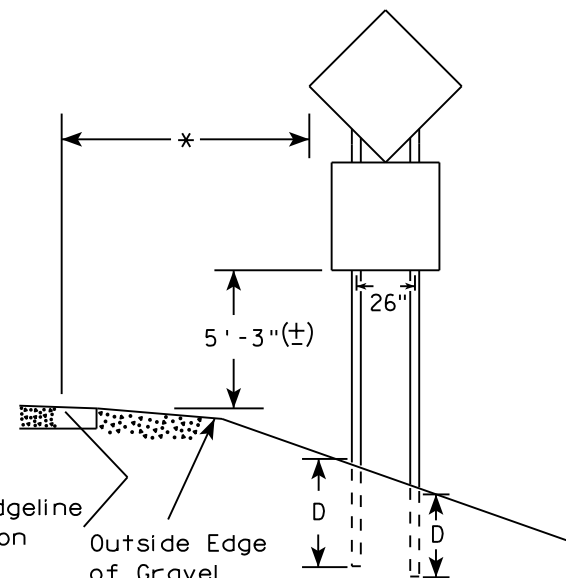
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

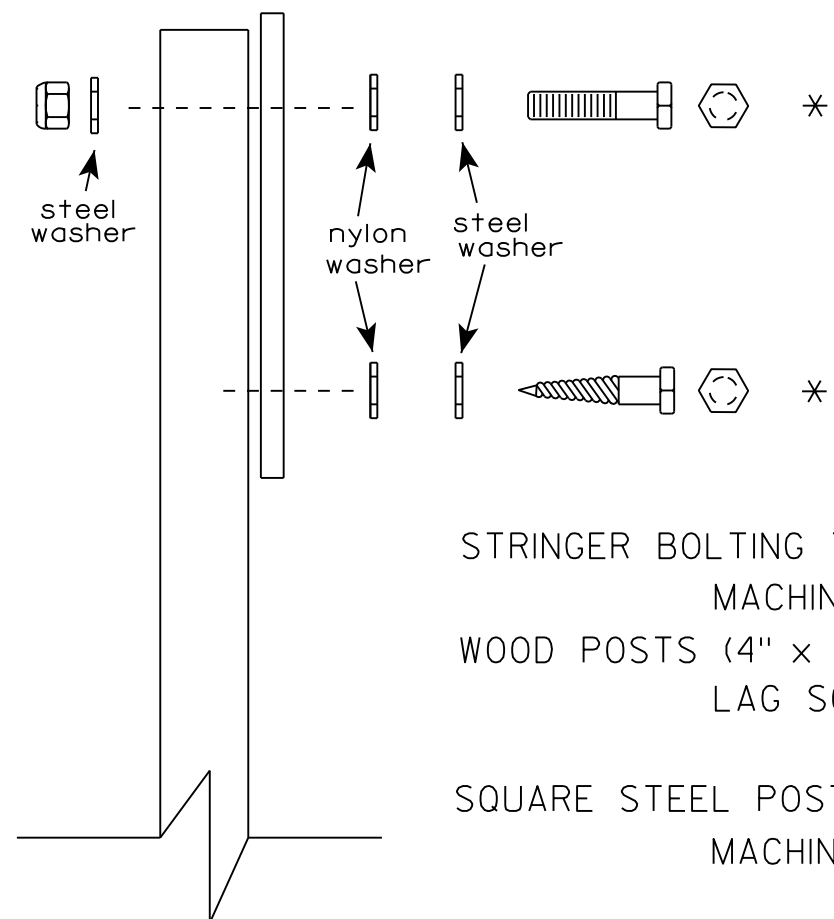
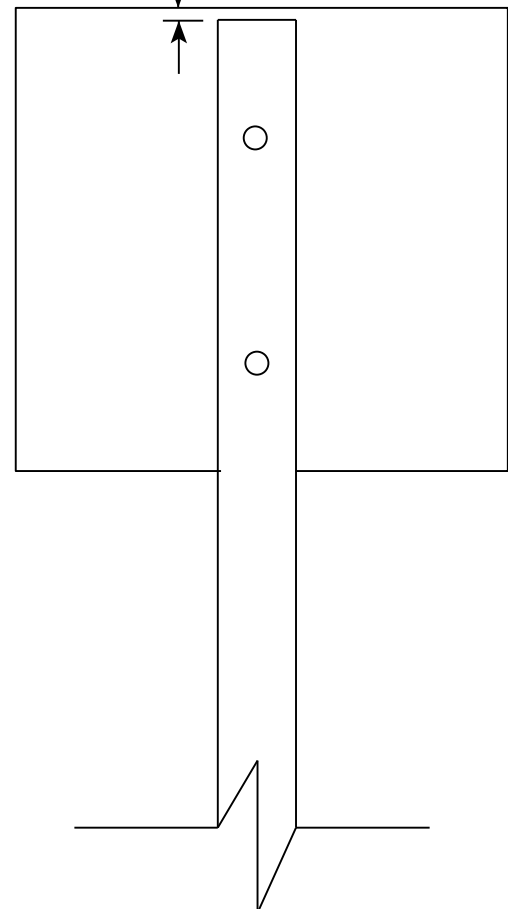
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15

1"± 1/2"

SIGN SHALL BE MOUNTED TO PROJECT ABOVE THE TOP OF THE POST



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
 - 3/8" X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - 3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 - 1-1/4" O.D. X 3/8" I.D. X .080 NYLON

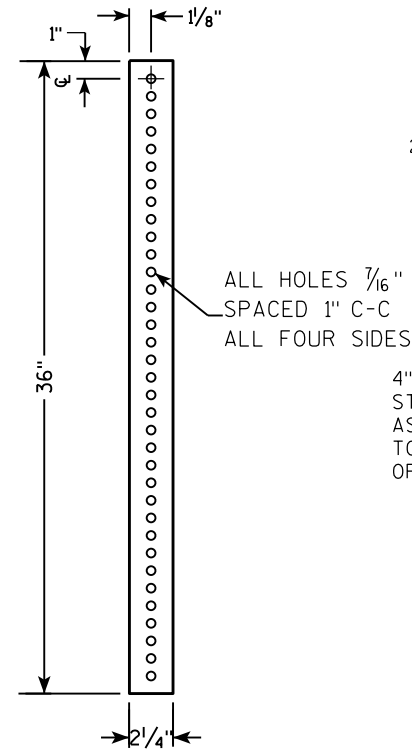
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

7

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 8/11/16	PLATE NO. A4-8.8

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

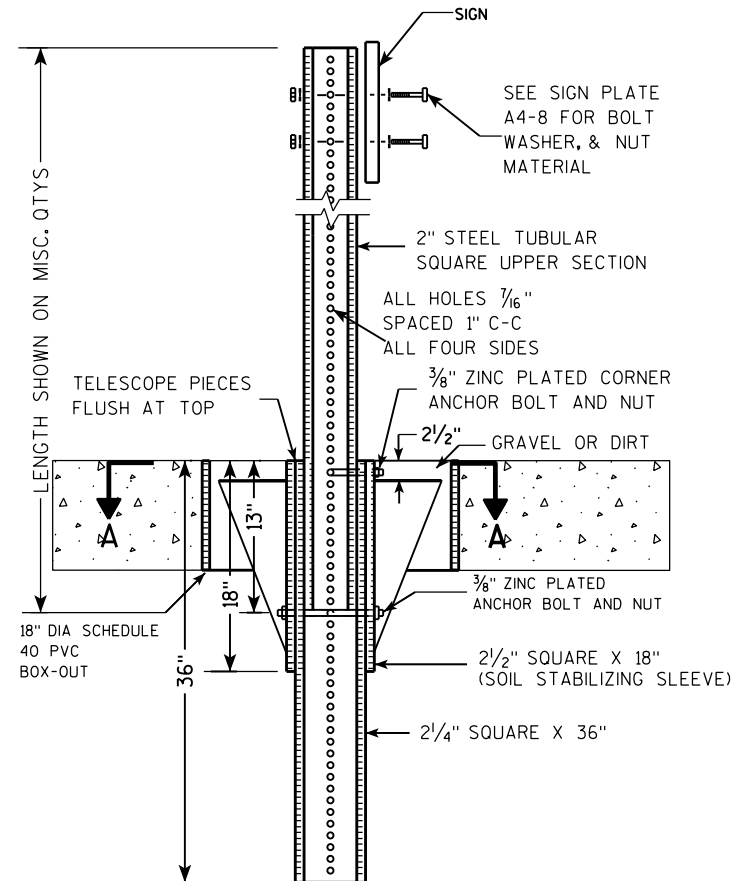
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



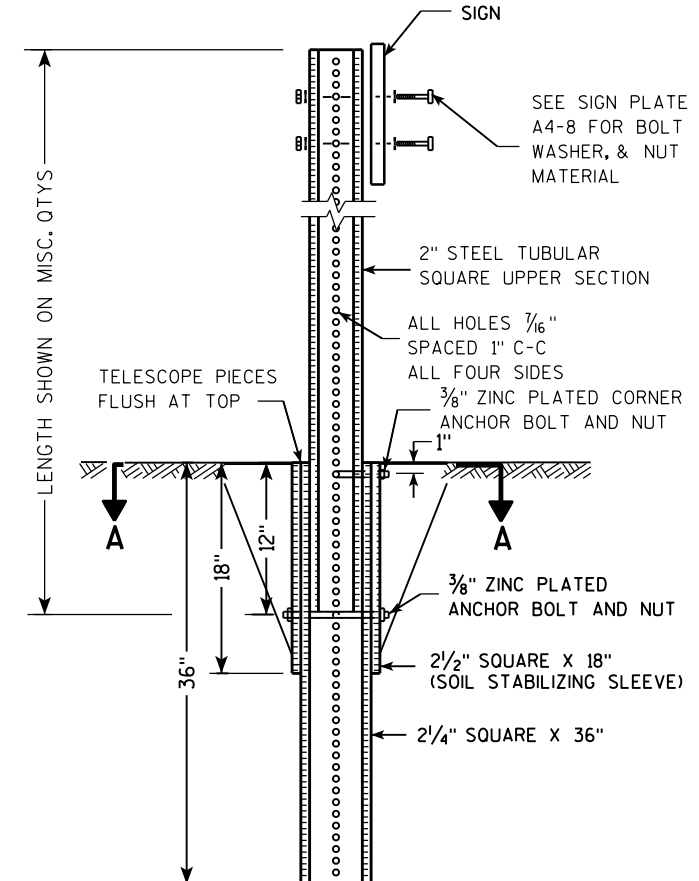
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

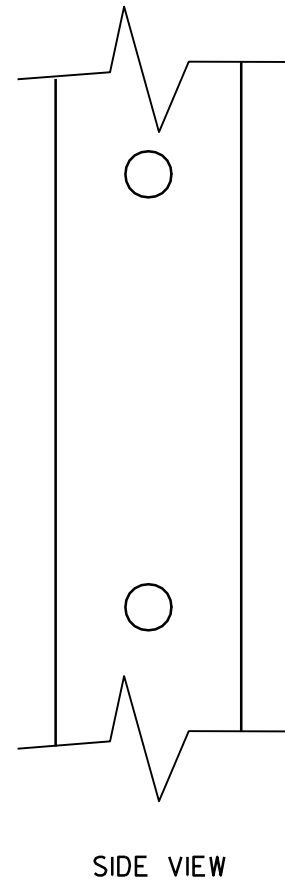
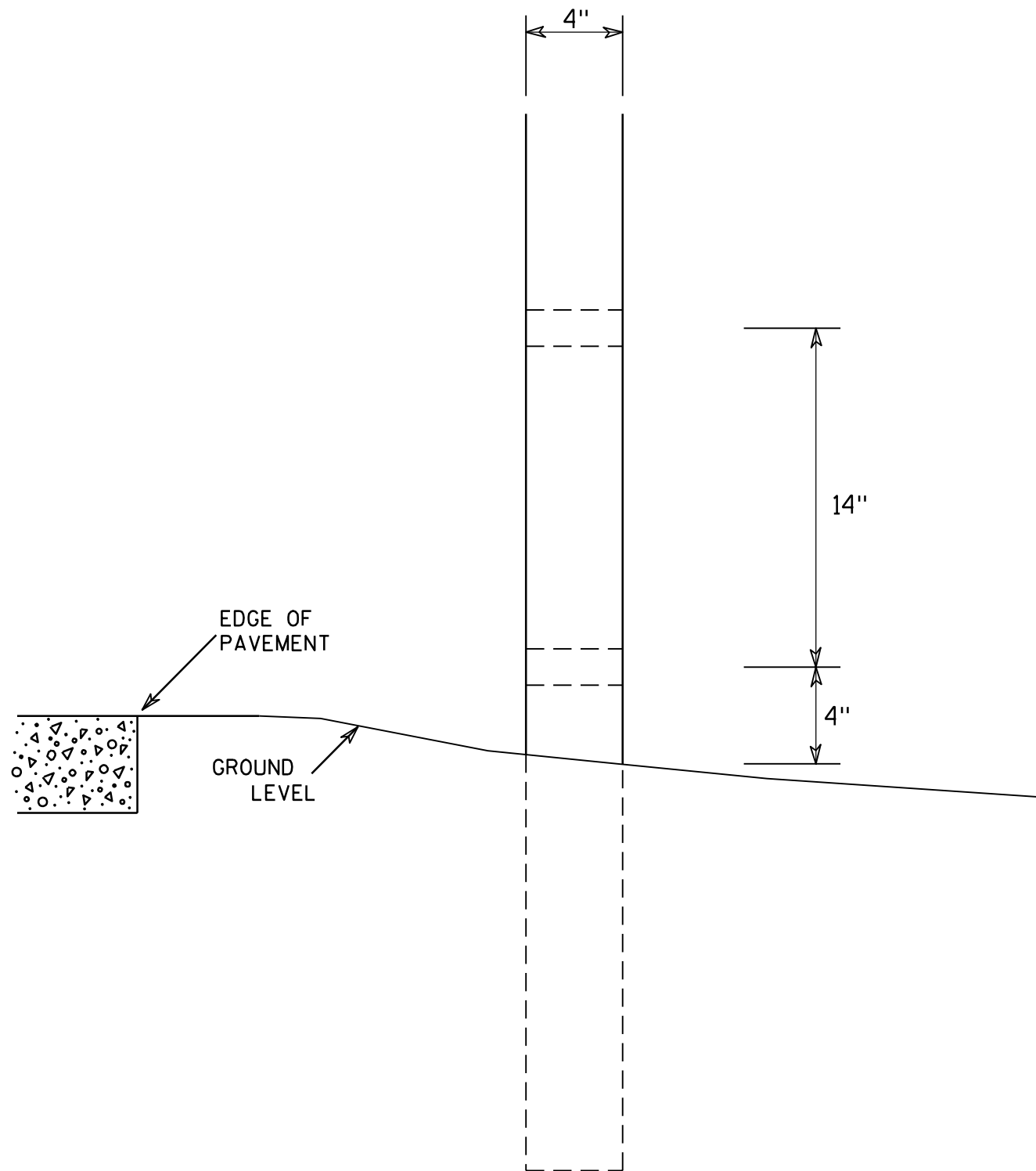
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9



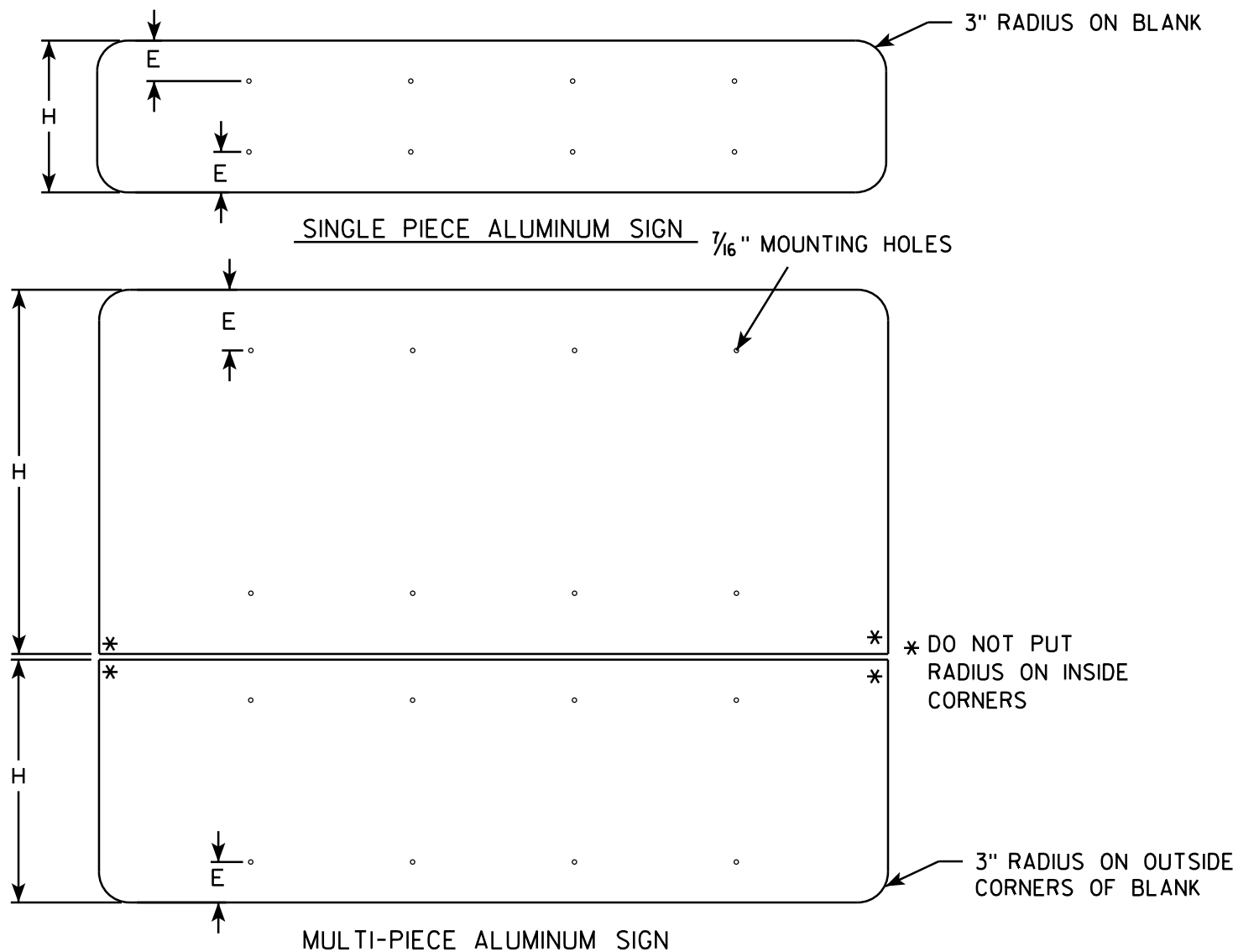
GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

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4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

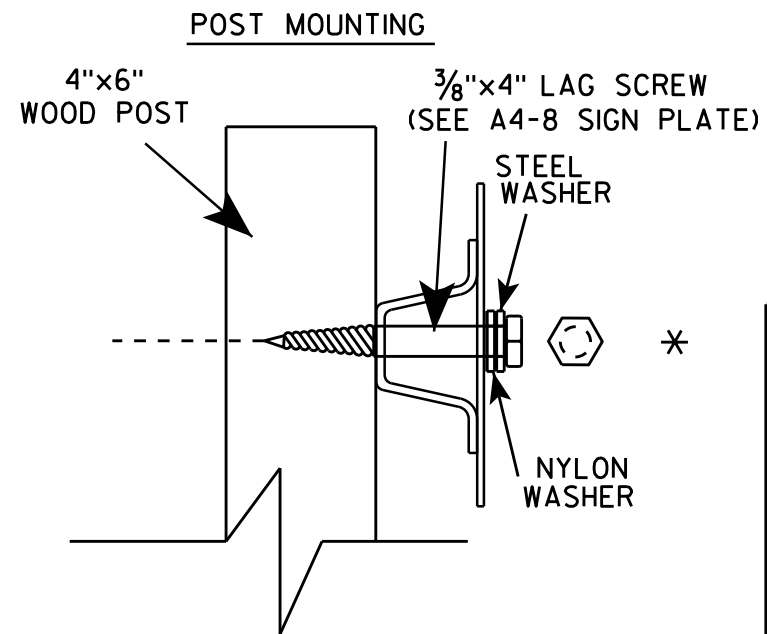
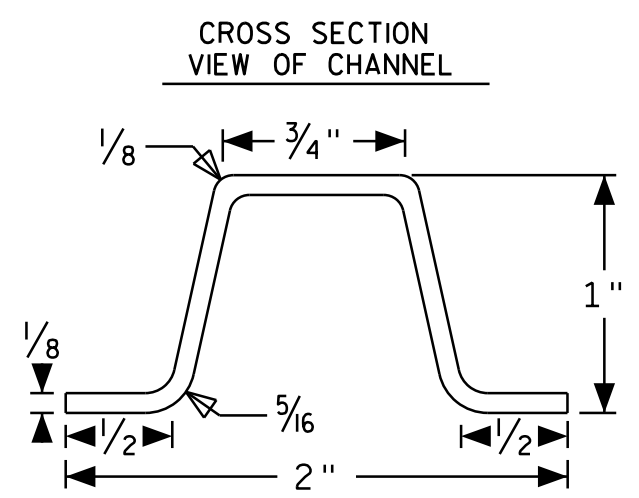
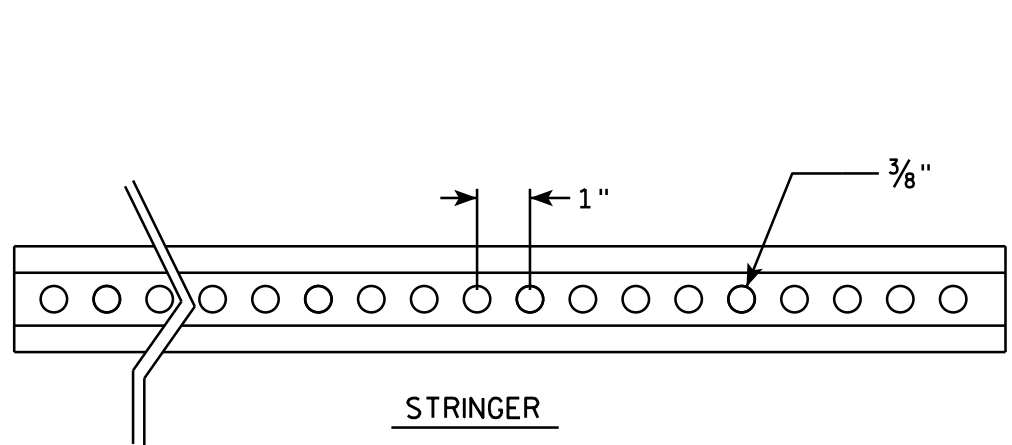


GENERAL NOTES

- ALL SIGNS OVER 60" IN WIDTH SHALL HAVE A 3" RADIUS ON THE OUTSIDE CORNERS OF THE ALUMINUM BLANK.
- MOUNTING HOLES SHALL BE 7/16" DIAMETER.
- SEE CHART FOR HOLE SPACING REQUIREMENTS
- FOR SIGN PANELS WITH DIMENSION (H) 36" AND OVER, DIMENSION E SHALL BE 6"
- FOR SIGN PANELS WITH DIMENSION (H) UNDER 36", DIMENSION E SHALL BE 4"
- SIGN STRINGER MATERIAL SHALL CONSIST OF STEEL CHANNEL POST SECTIONS, WEIGHING 1.12 LBS/FT IN ACCORDANCE WITH SECTION 633.2.1 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- SEE SIGN PLATE A4-8 FOR SIGN STRINGER BOLTING REQUIREMENTS.

SIGN WIDTH	STRINGER WIDTH	POSTS	HOLE SPACING	MOUNTING HOLES
78"	72"	2	16"	15" 31" 47" 63"
84"	72"	2	17"	16 1/2" 33 1/2" 50 1/2" 67 1/2"
90"	72"	2	18"	18" 36" 54" 72"
96"	90"	2	19"	19 1/2" 38 1/2" 57 1/2" 76 1/2"
102"	90"	2	20"	21" 41" 61" 81"
108"	90"	2	21"	22 1/2" 43 1/2" 64 1/2" 85 1/2"
114"	108"	3	15"	12" 27" 42" 57" 72" 87" 102"
120"	108"	3	16"	12" 28" 44" 60" 76" 92" 108"
126"	108"	3	17"	12" 29" 46" 63" 80" 97" 114"
132"	126"	3	18"	12" 30" 48" 66" 84" 102" 120"
138"	126"	3	19"	12" 31" 50" 69" 88" 107" 126"
144"	126"	3	20"	12" 32" 52" 72" 92" 112" 132"

7



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SIGN STRINGER MOUNTING REQUIREMENTS

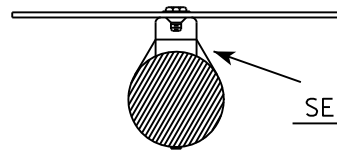
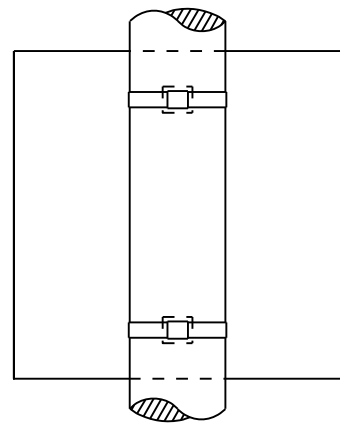
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/26/16 PLATE NO. A4-18.1

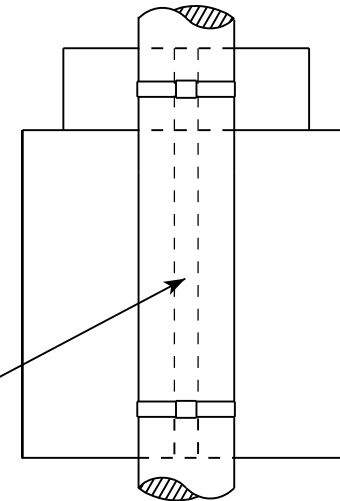
BANDING

SINGLE SIGN

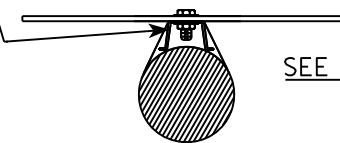


SEE DETAIL A

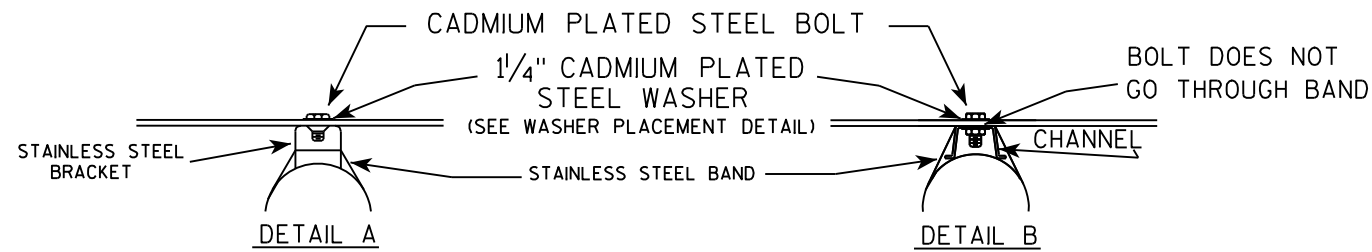
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



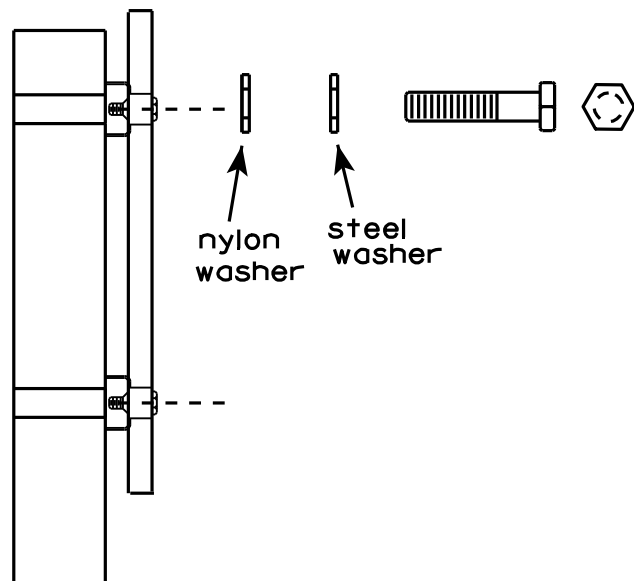
SEE DETAIL B



GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.

WASHER PLACEMENT



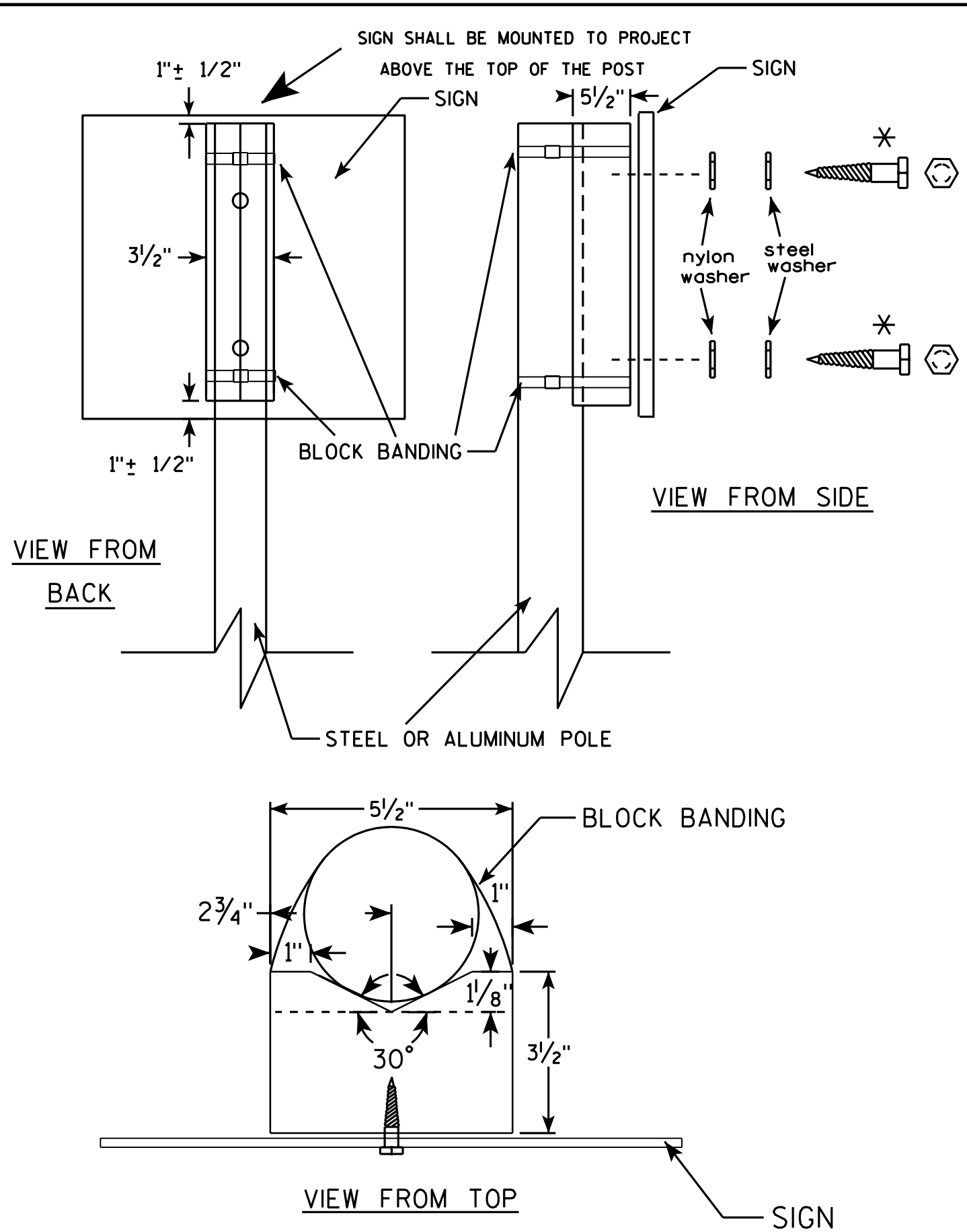
WASHERS (ALL POSTS) -
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON
 FOR ALL TYPE H SIGNS

STANDARD SIGN
 SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
 for State Traffic Engineer

DATE 8/16/13 PLATE NO. A5-9.3



GENERAL NOTES

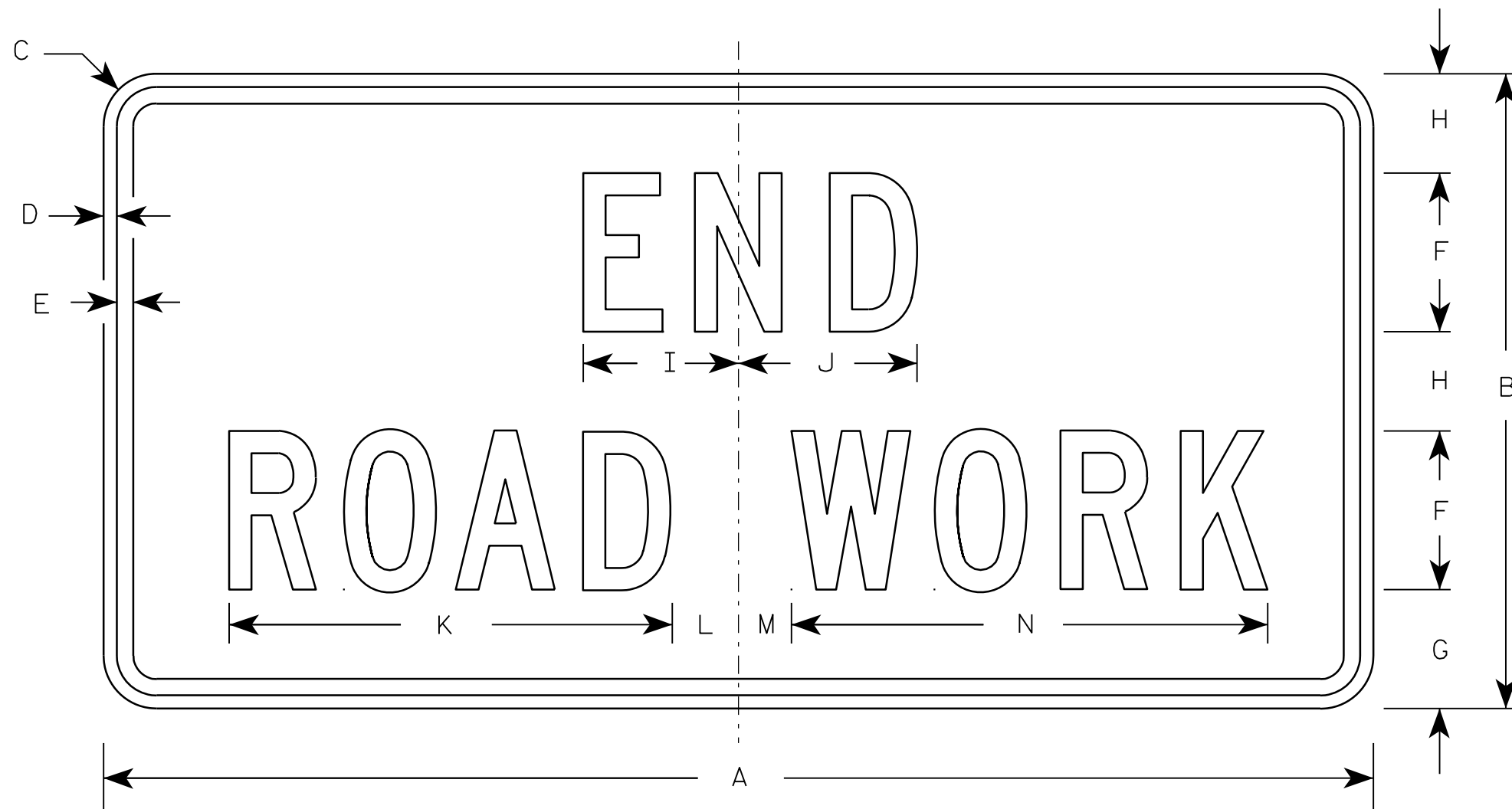
1. WOOD 4"x6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D, or
 - b. Cadmium plated in accordance with ASTM Designation : B 766 TYPE 3, Class 12, or
 - c. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

* LAG BOLTS SHALL BE 3/8" X 2 1/2"

BLOCK BANDING DETAIL (V-BLOCK OPTION)	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 7/12/07	PLATE NO. A5-10.1

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



G20-2A

7

7

Metric equivalent for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

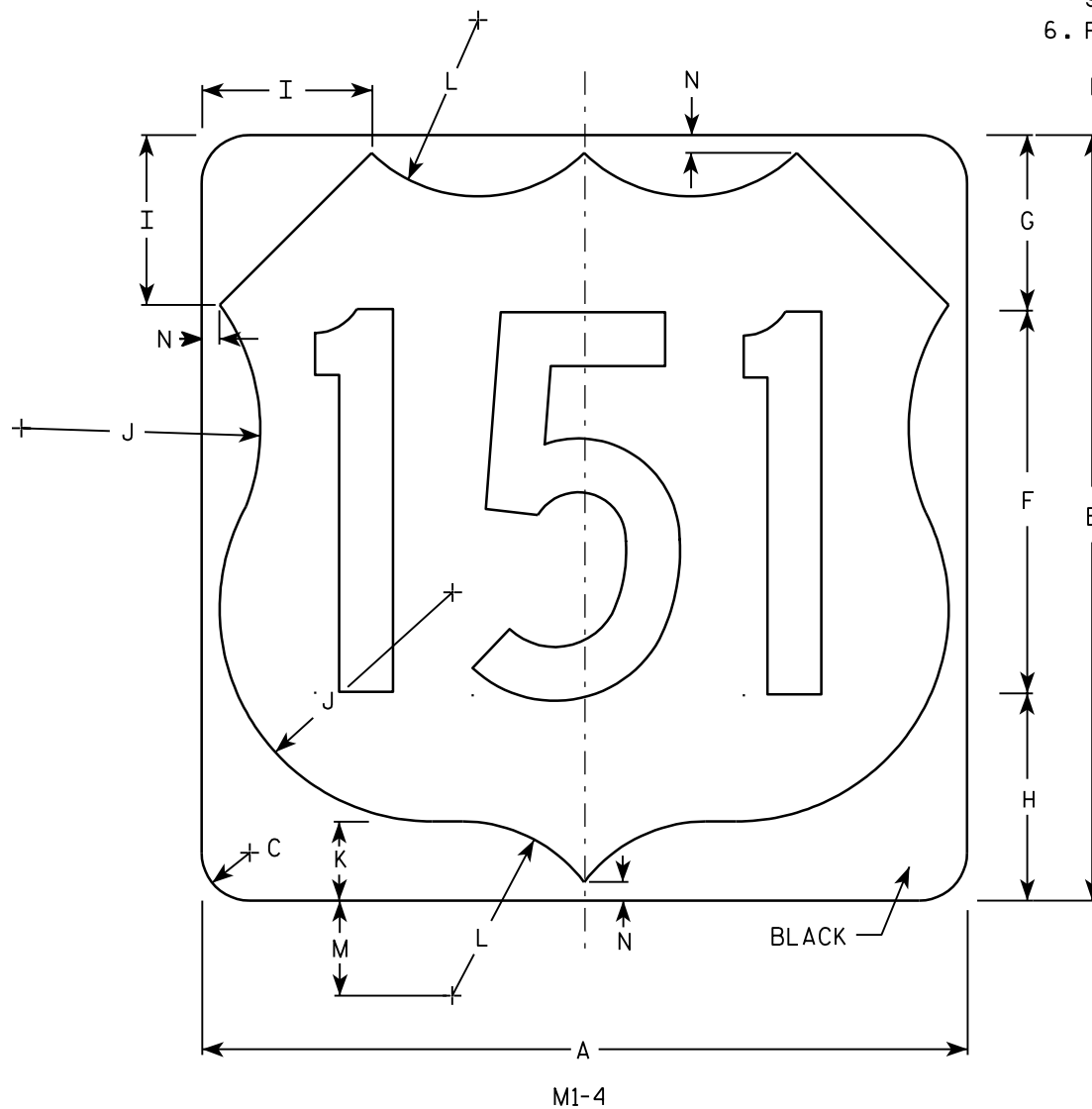
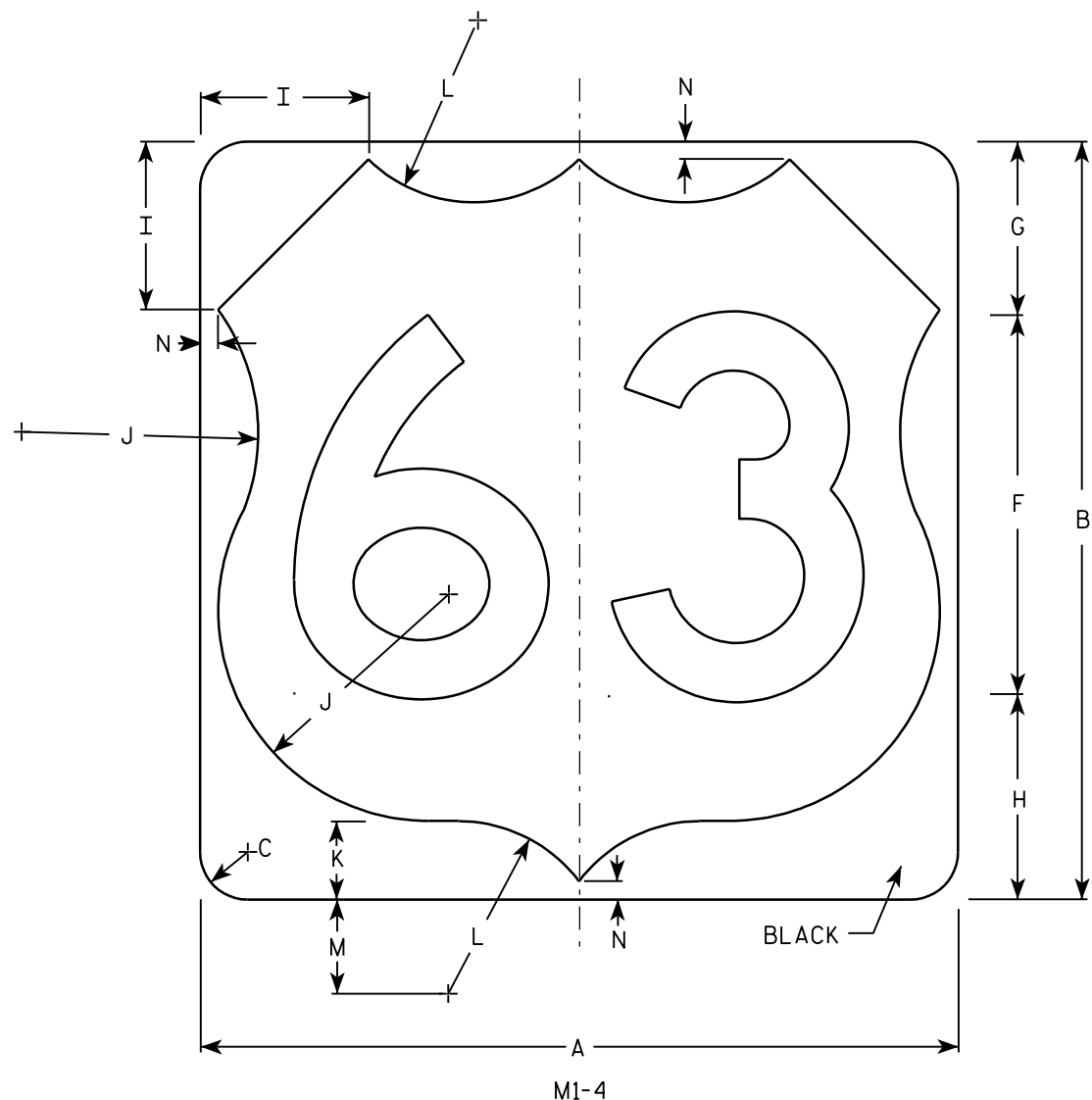
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area sq.
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

STANDARD SIGN G20-2A	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 9/30/09	PLATE NO. G20-2A.8

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 6
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and adjust spacing as per Plate A10-1.
6. Permanent Signs
Background - Type H Reflective
Detour or other temporary signs
Background - Reflective



Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0	.36
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81

USH MARKER
M1-4 FOR ASSEMBLIES

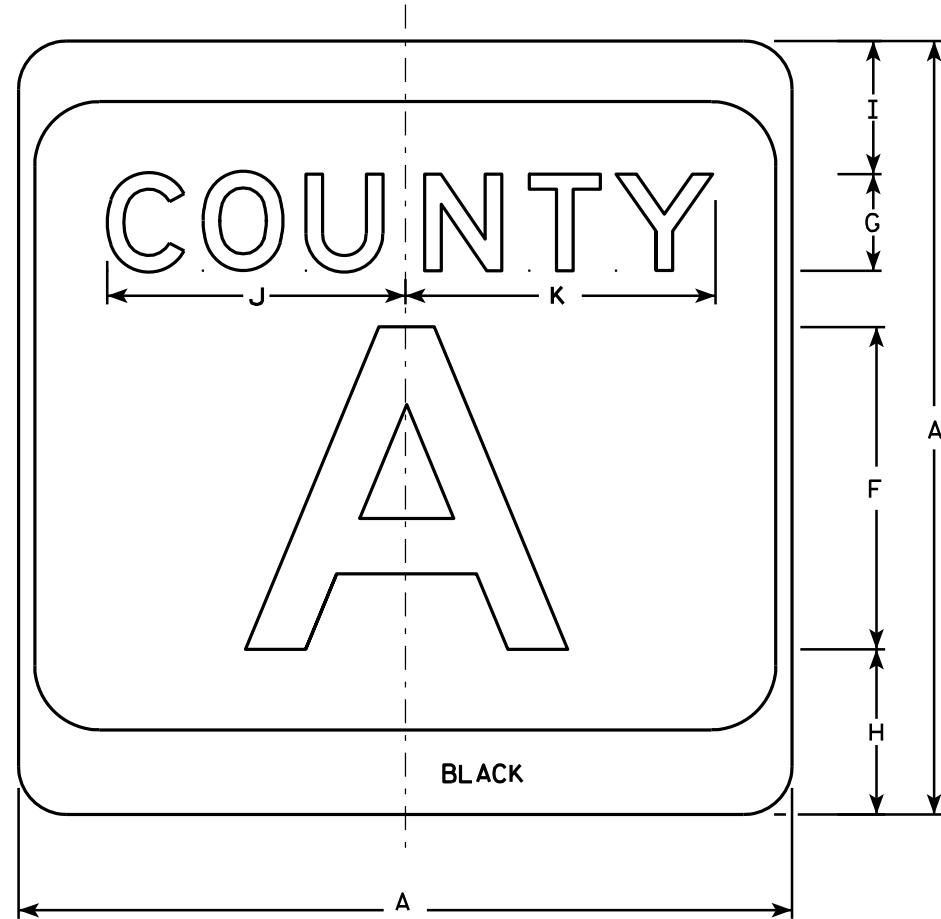
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

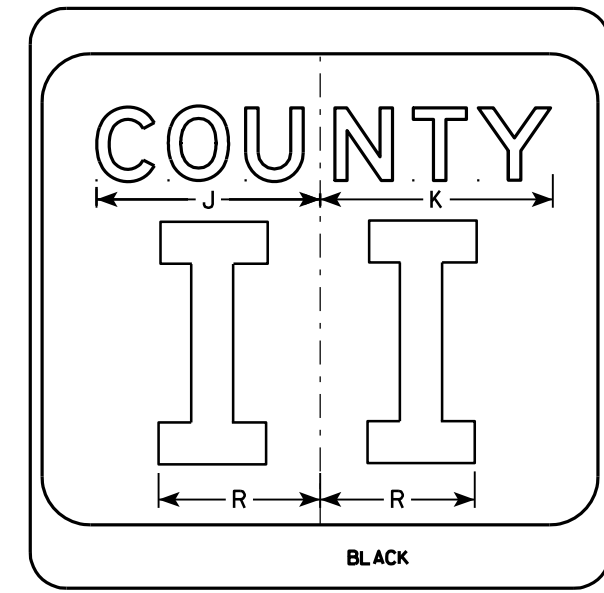
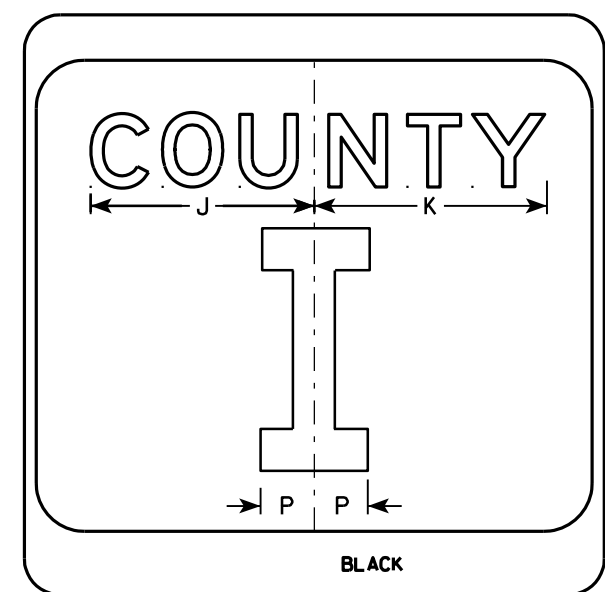
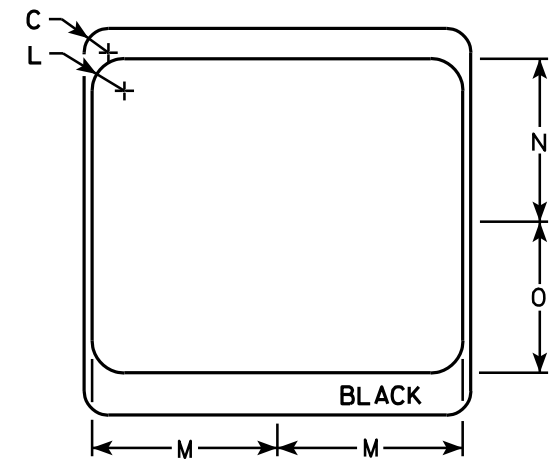
DATE 08/25/05 PLATE NO. M1-4.9

NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 7
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
Message Series D for 2 letters unless message is too big then Series C.
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



M1-5A



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

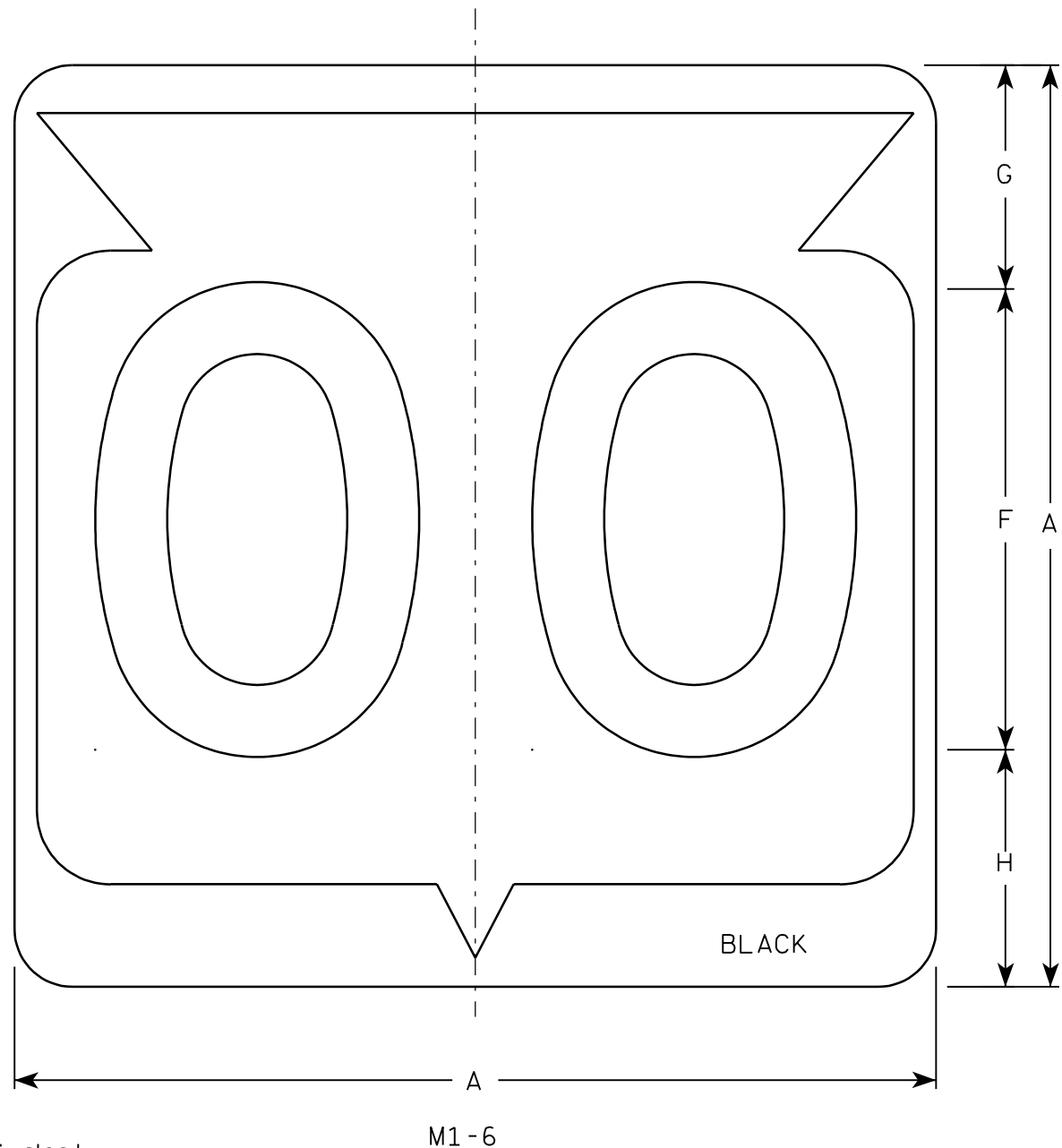
CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*
For State Traffic Engineer

DATE 9/27/11 PLATE NO. MI-5A.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



M1-6

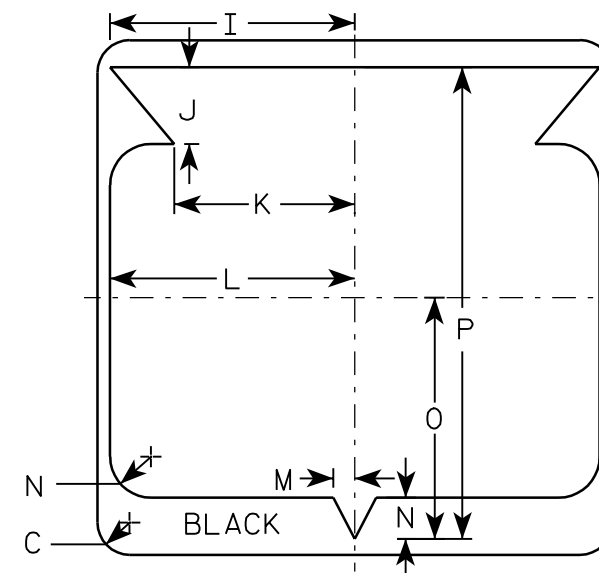
Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81

NOTES

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 6
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
6. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



7

7

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

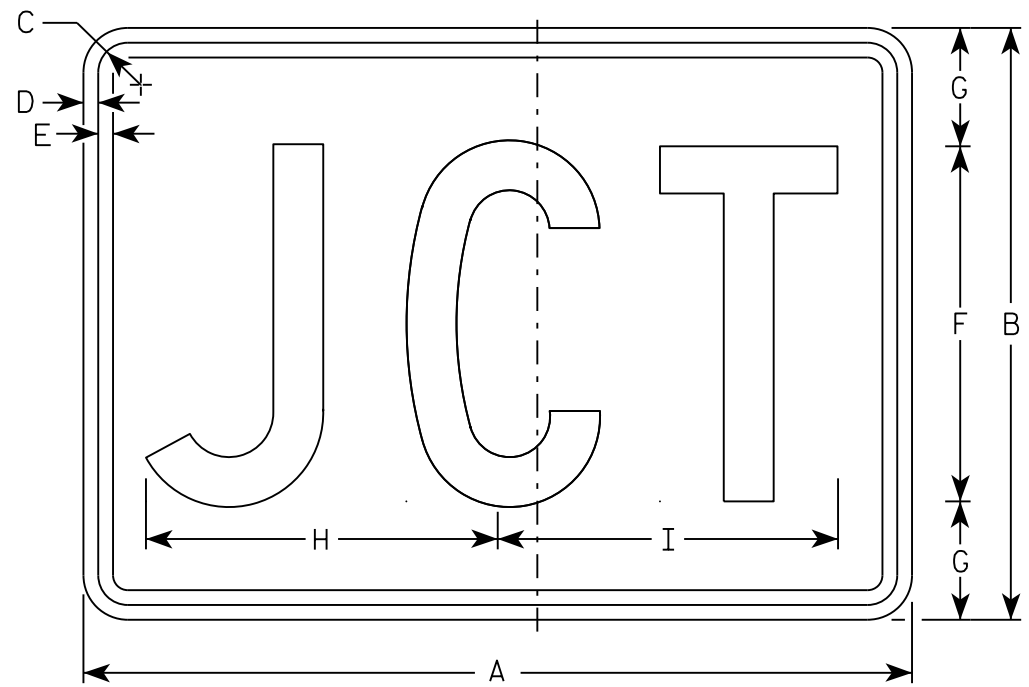
APPROVED *Chester J. Spang*
for State Traffic Engineer

DATE 3/20/02 PLATE NO. M1-6.9

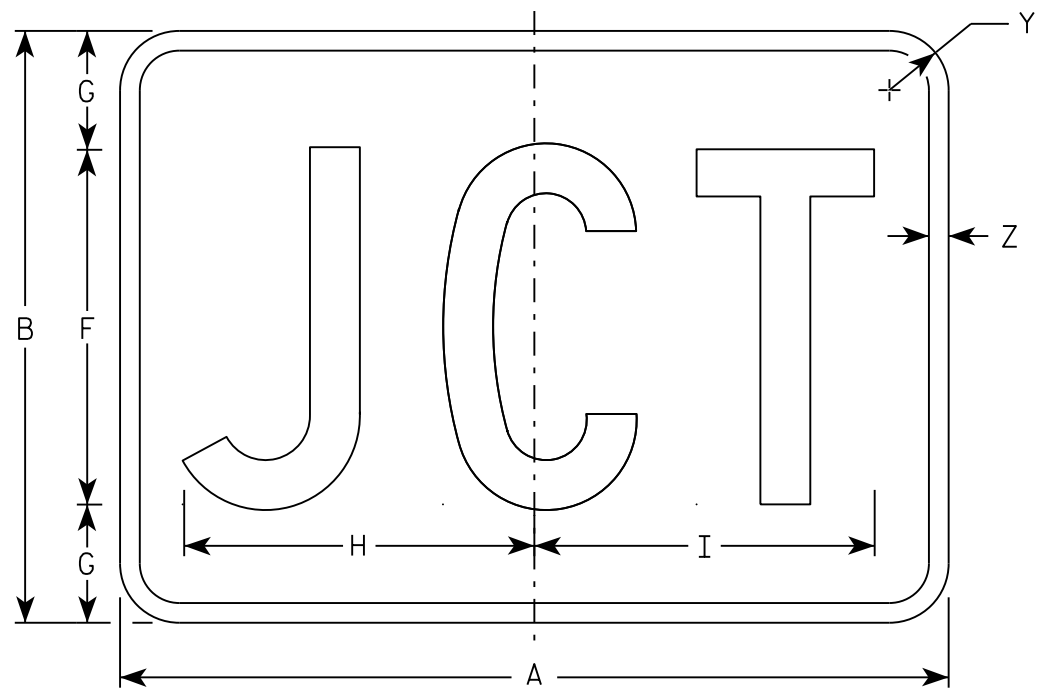
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H
2. Color:
 - Background - See note 5
 - Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White
 Message - Black
 MB2-1 Background - Blue
 Message - White
 MK2-1 Background - Green
 Message - White
 MM2-1 Background - White
 Message - Green
 MN2-1 Background - Brown
 Message - White
 MP2-1 Background - White
 Message - Blue
 MR2-1 Background - Brown
 Message - Yellow



M2-1
MM2-1
MP2-1



MB2-1
MK2-1
MN2-1
MR2-1

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

STANDARD SIGN
M2-1

WISCONSIN DEPT OF TRANSPORTATION

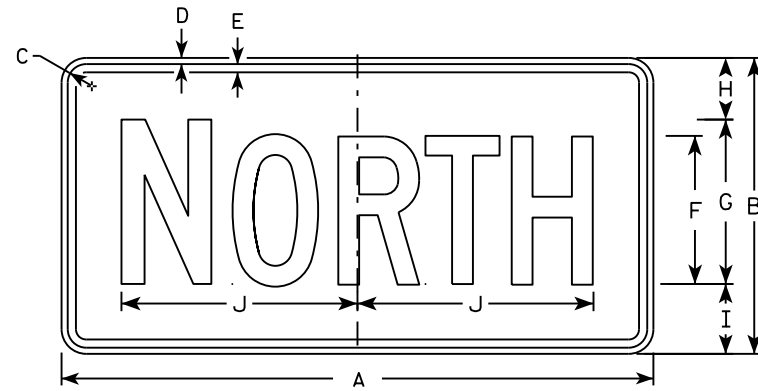
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 10/15/15 PLATE NO. M2-1.12

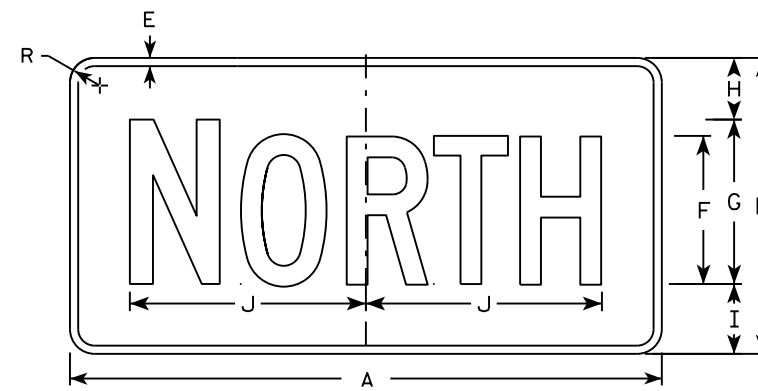
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

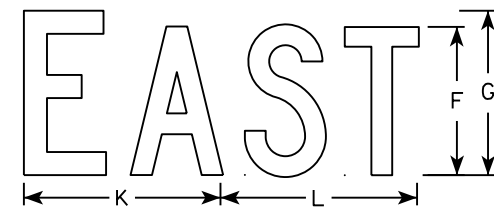
- All Signs Type II - Type H
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
 Message - Black
 MB3-1 thru MB3-4 Background - Blue
 Message - White
 MK3-1 thru MK3-4 Background - Green
 Message - White
 MM3-1 thru MM3-4 Background - White
 Message - Green
 MN3-1 thru MN3-4 Background - Brown
 Message - White
 MP3-1 thru MP3-4 Background - White
 Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.



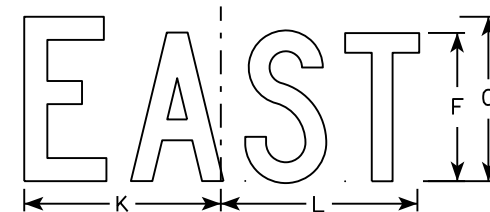
M3-1
MM3-1
MP3-1



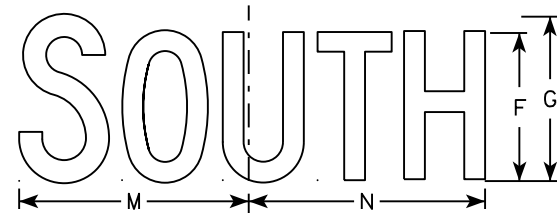
MB3-1
MK3-1
MN3-1



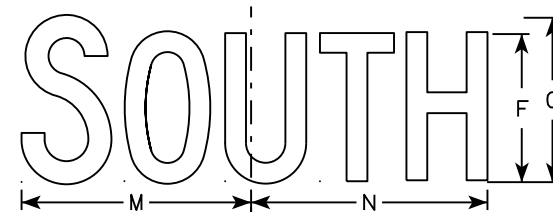
M3-2
MM3-2
MP3-2



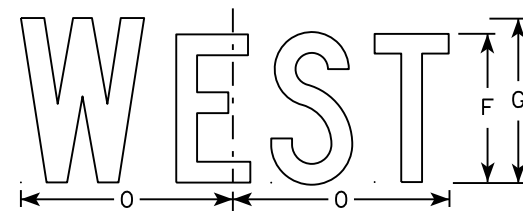
MB3-2
MK3-2
MN3-2



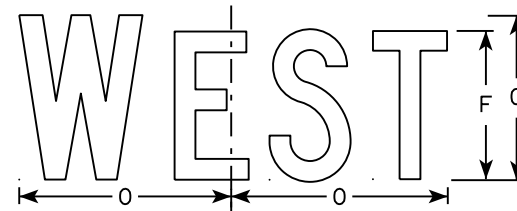
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

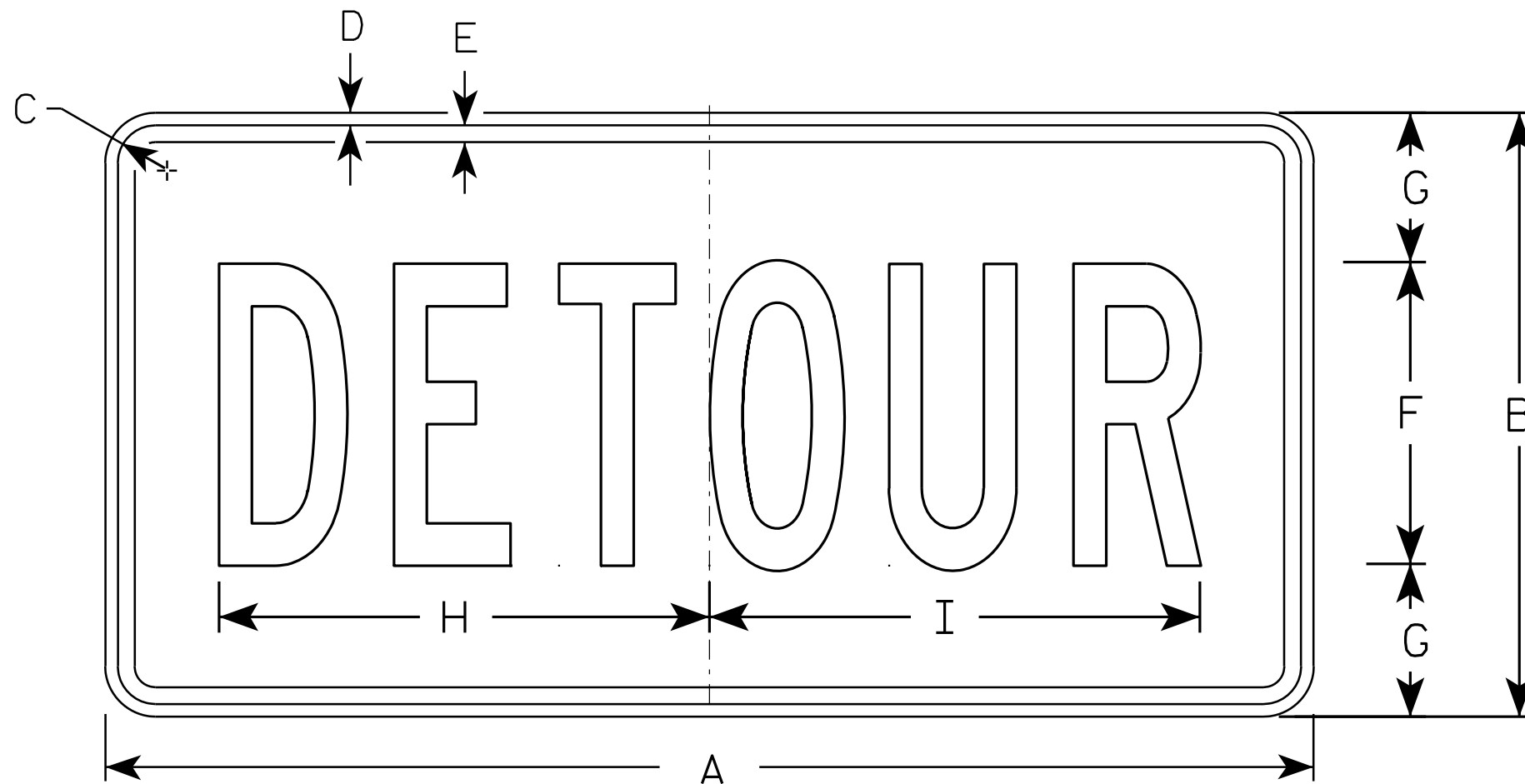
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

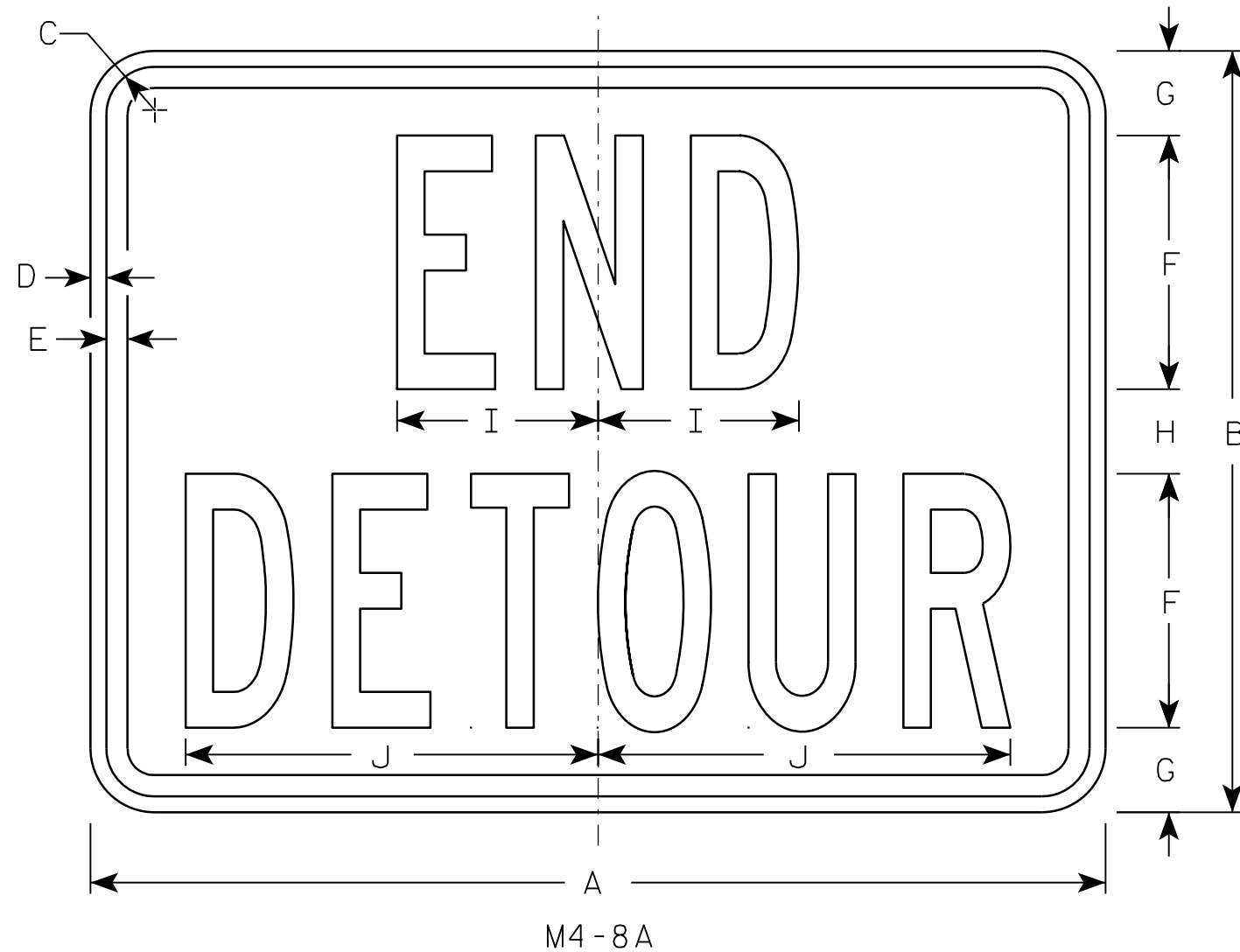
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

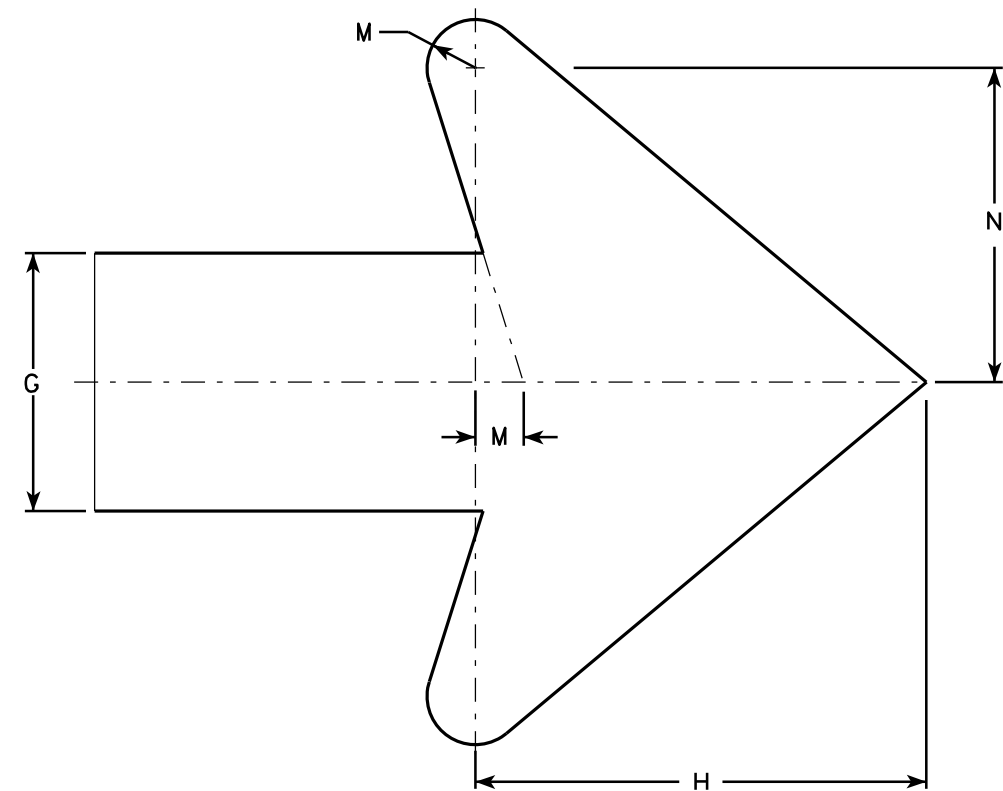
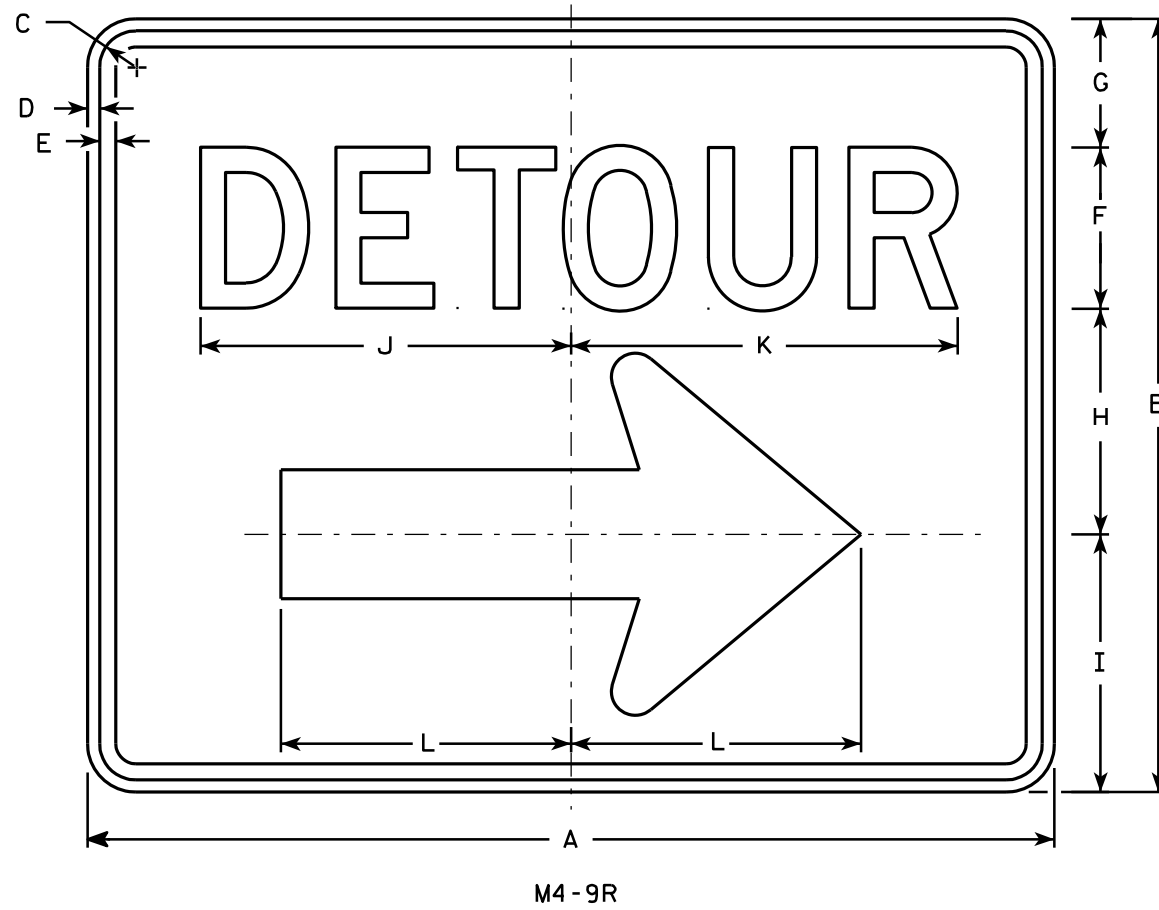
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

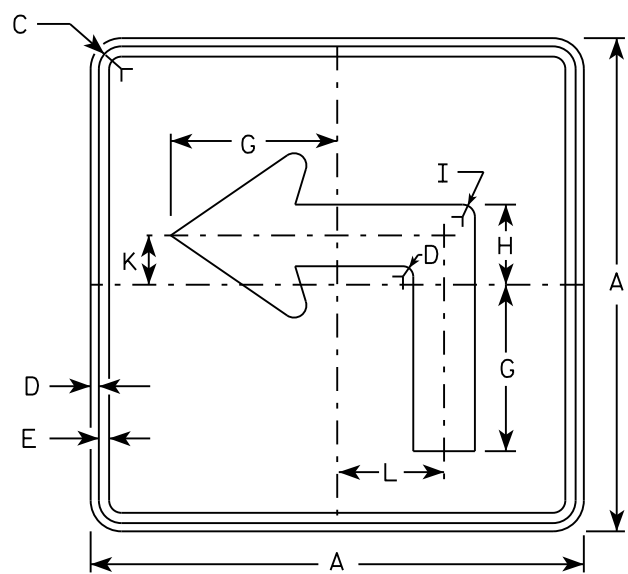
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

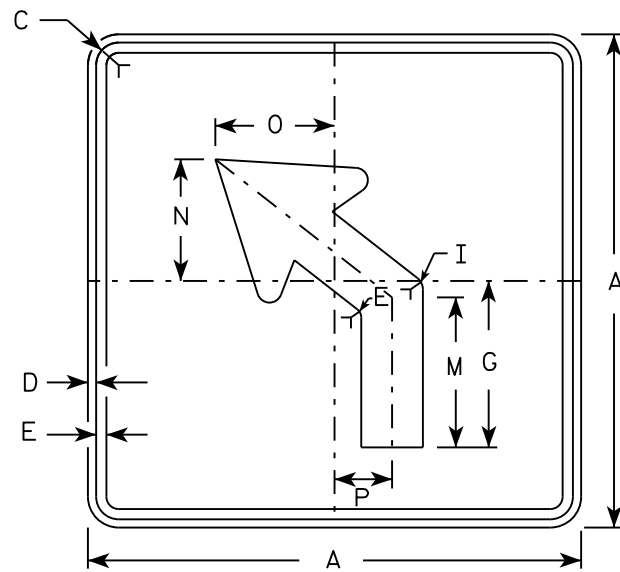
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

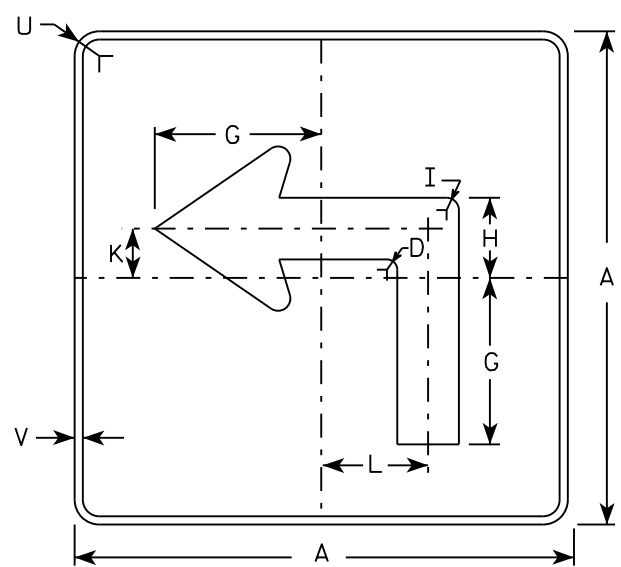
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



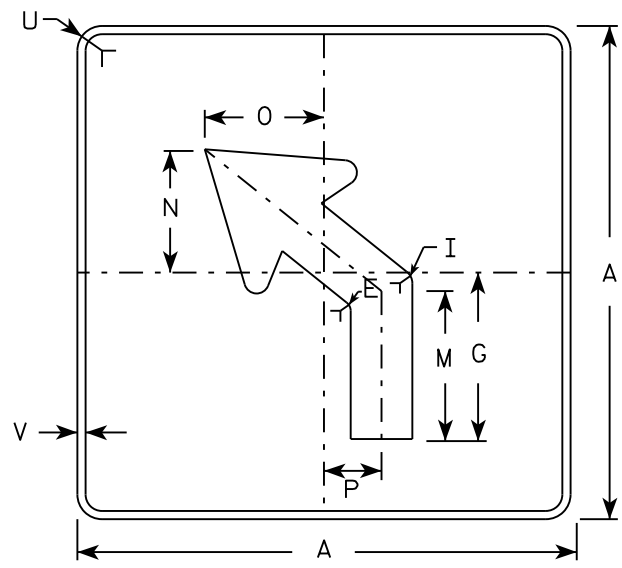
M5-1L
MM5-1L
M05-1L
MP5-1L



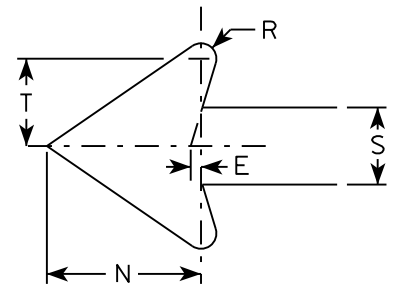
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|---|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White - Type H Reflective |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

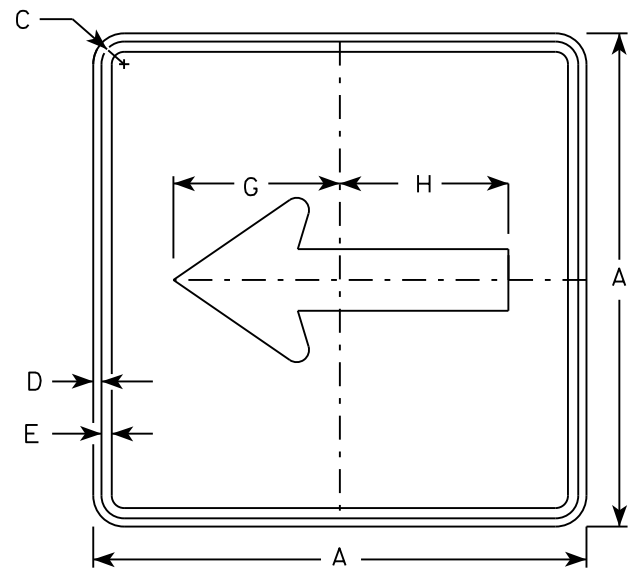
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

STANDARD SIGN
M5-1 & M5-2

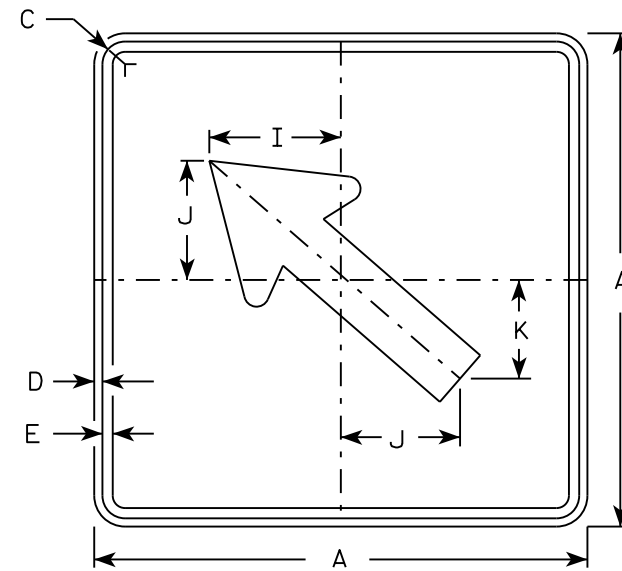
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

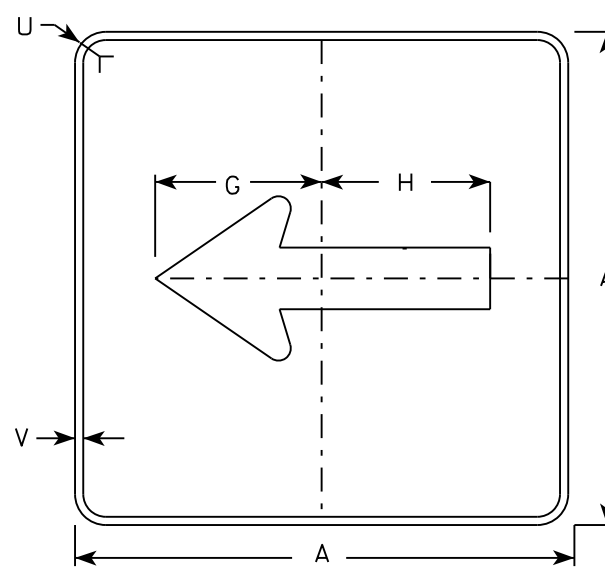
DATE 10/15/15 PLATE NO. M5-1.13



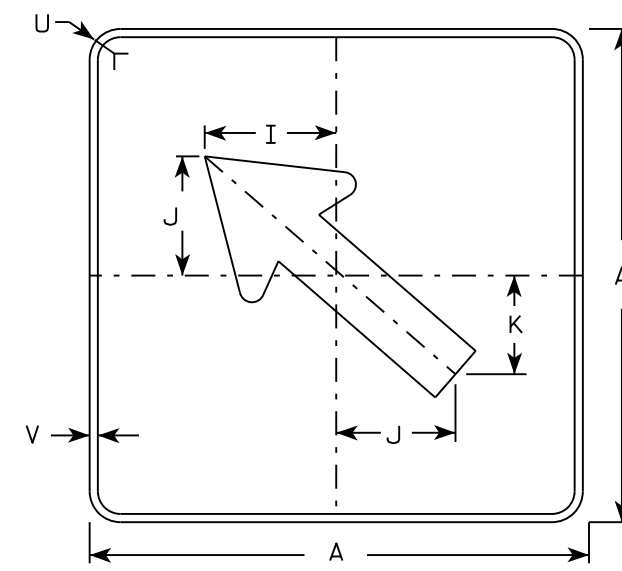
M6-1
MM6-1
M06-1
MP6-1



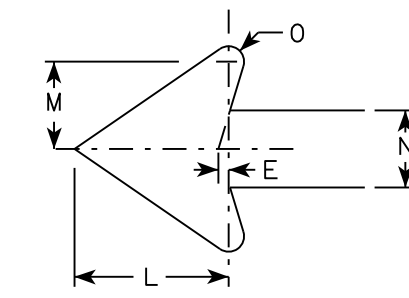
M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

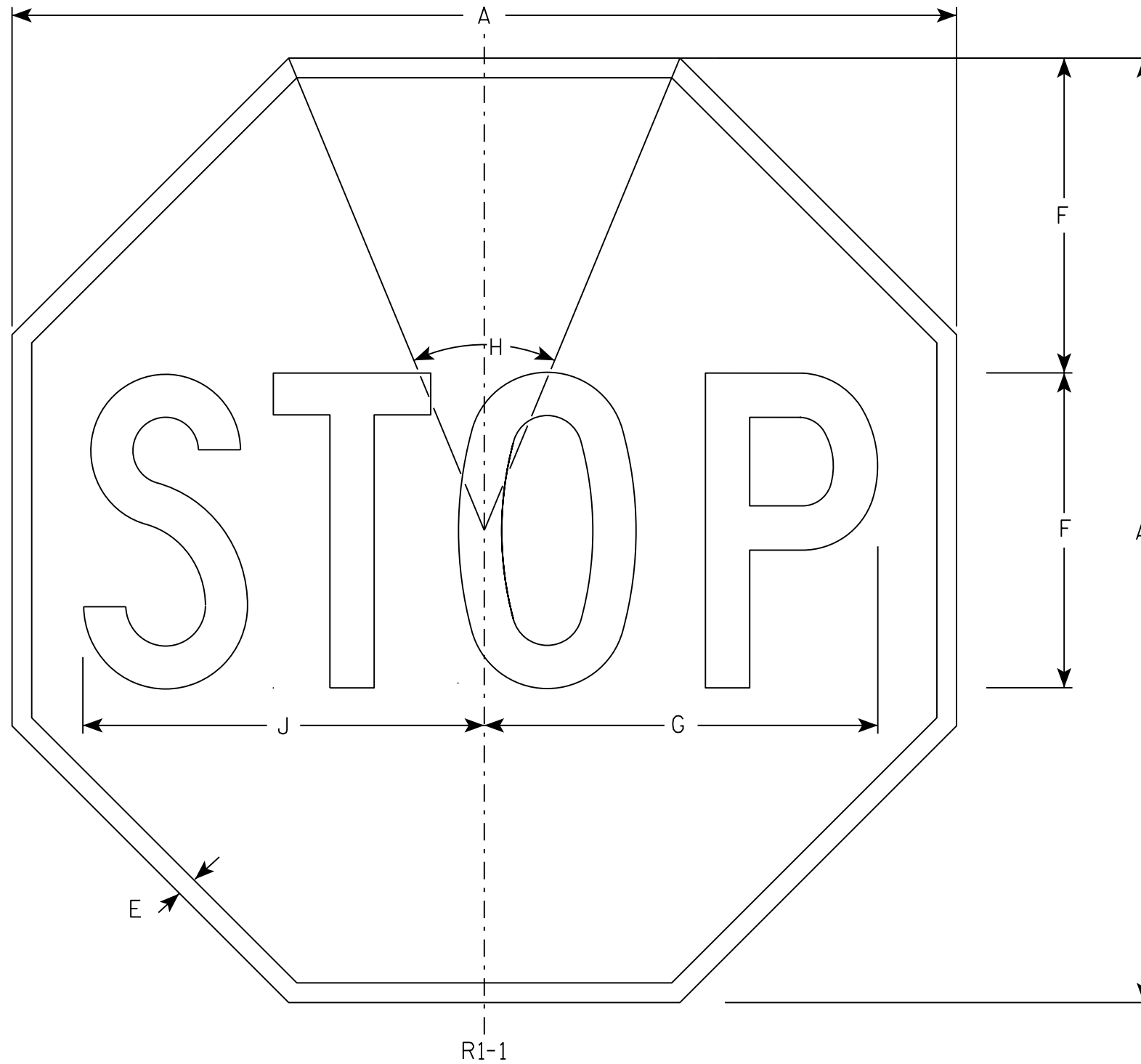
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

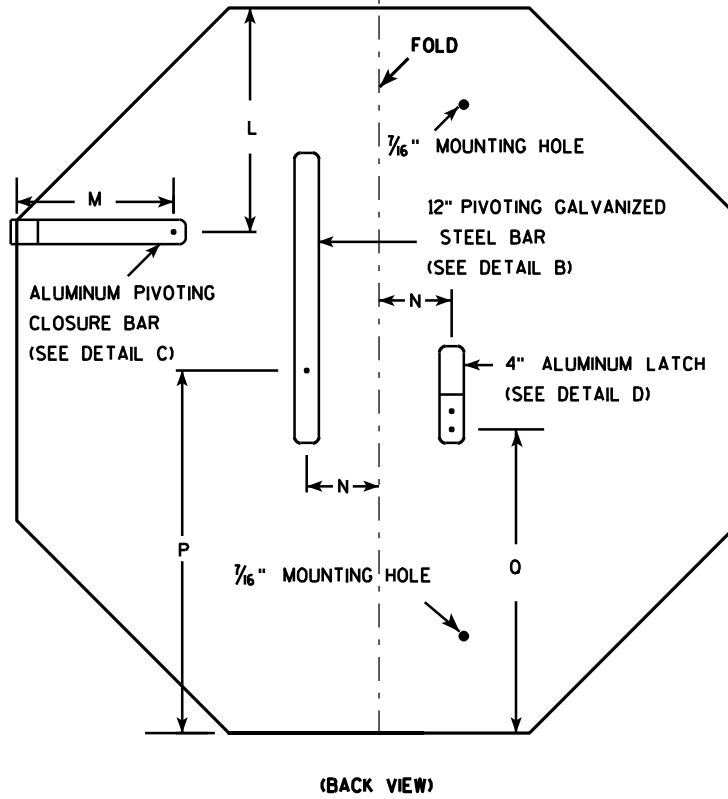
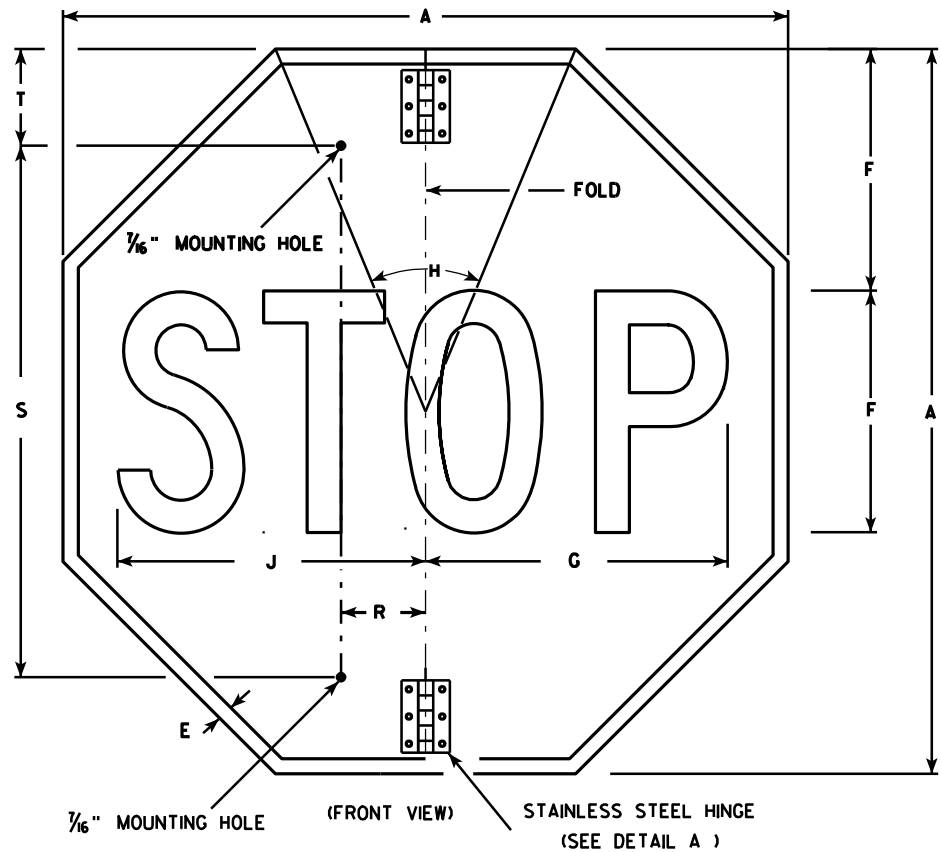
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

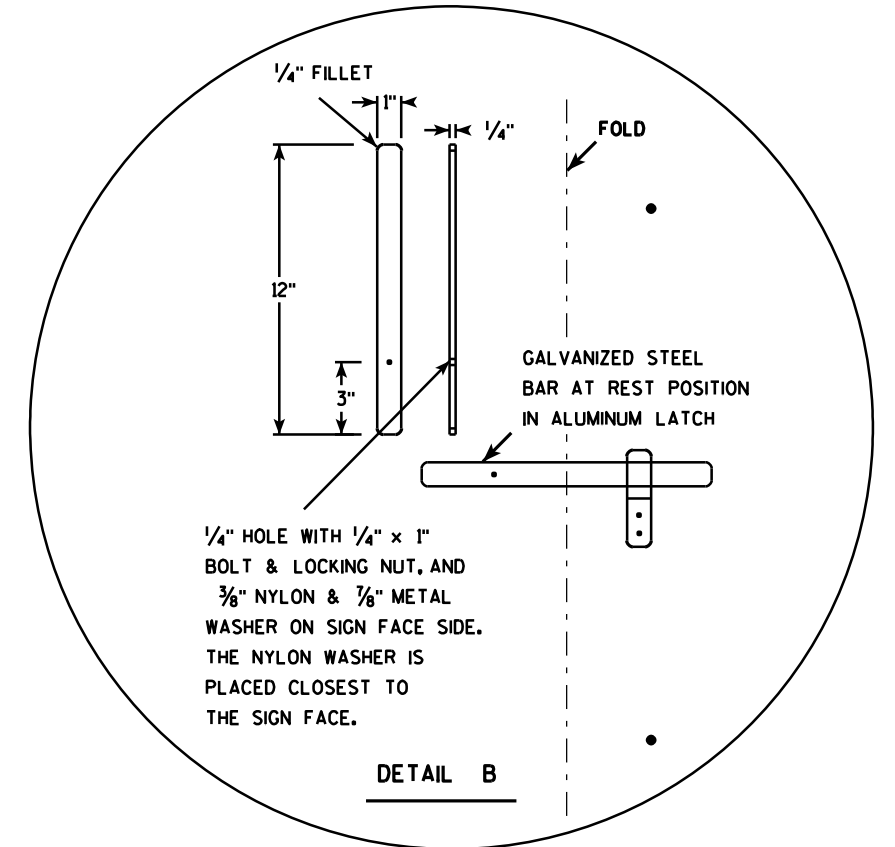
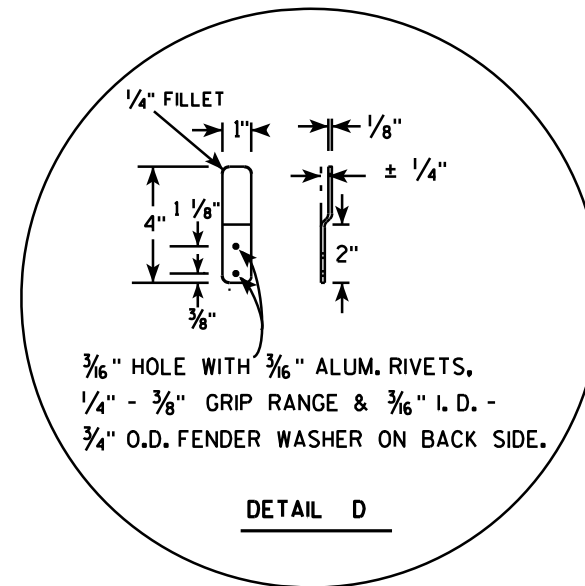
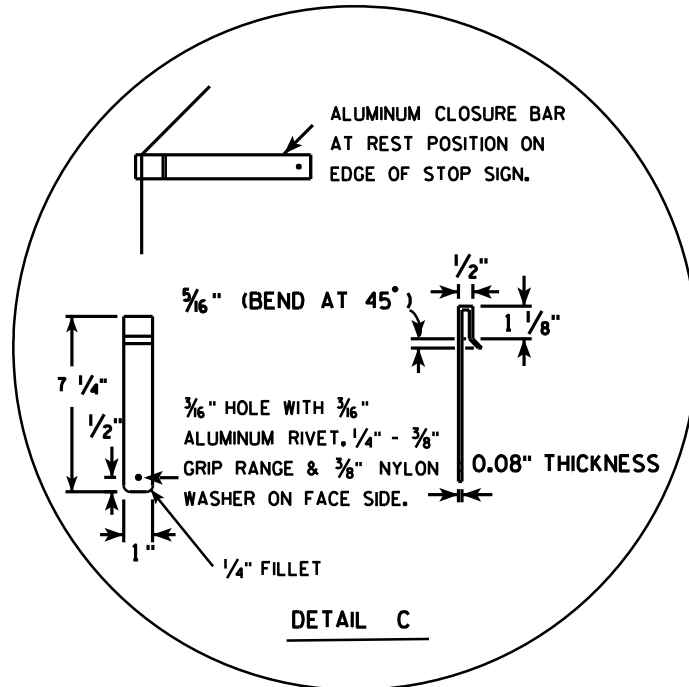
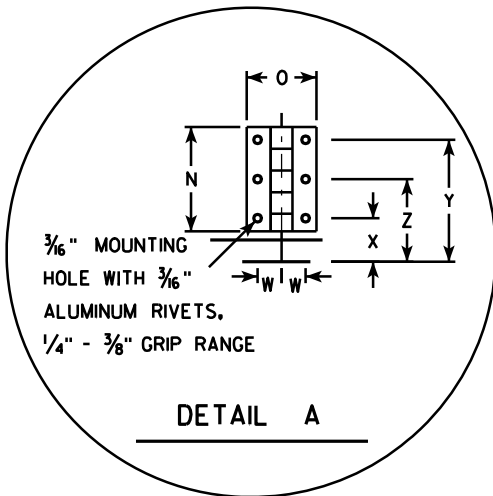
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C
4. All hardware used on the folding STOP sign installation shall conform to 637.2.4 of the WIS DOT Standard Specification.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30				5/8	10	12 1/2	45		12 3/4		9 1/4	6 1/2	3	2	15	12 3/8	2 1/2	22	5			1 1/8	1 1/4	3 1/2	2 3/8	5.18
2M	36				3/4	12	15	45		15 3/8		11	6 1/2	3	2	18	15 3/8	2 1/2	26	5			1 1/8	1 1/4	3 1/2	2 3/8	7.46
3	36				3/4	12	15	45		15 3/8		11	6 1/2	3	2	18	15 3/8	2 1/2	26	5			1 1/8	1 1/4	3 1/2	2 3/8	7.46
4																											
5																											

STANDARD SIGN
R1-1F

WISCONSIN DEPT OF TRANSPORTATION

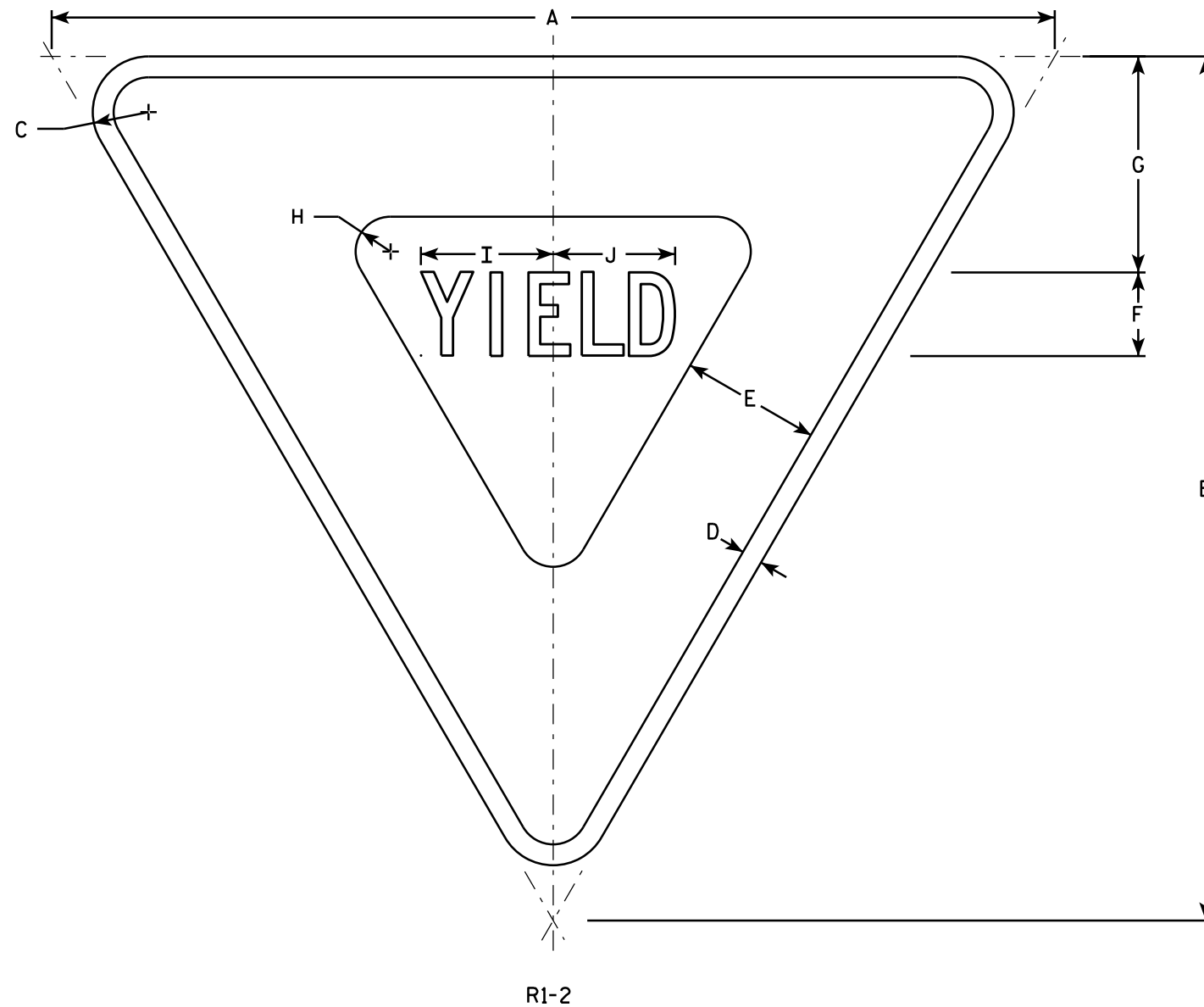
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1F.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The border strip and word message are reflectorized red.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	26	1 1/2	5/8	4	2 1/2	6 3/8	7/8	4	3 5/8																	2.71
2S	36	31	2	3/4	5	3	7 3/4	1 1/4	4 3/4	4 3/8																	3.88
2M	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
3	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
4	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
5	60	52	3	1 1/2	8	5	13	2 1/2	7 7/8	7 1/4																	10.83
6																											
7	18	15 1/2	1	3/8	2 1/2	1 1/2	3 7/8	5/8	2 3/8	2 1/4																	0.97

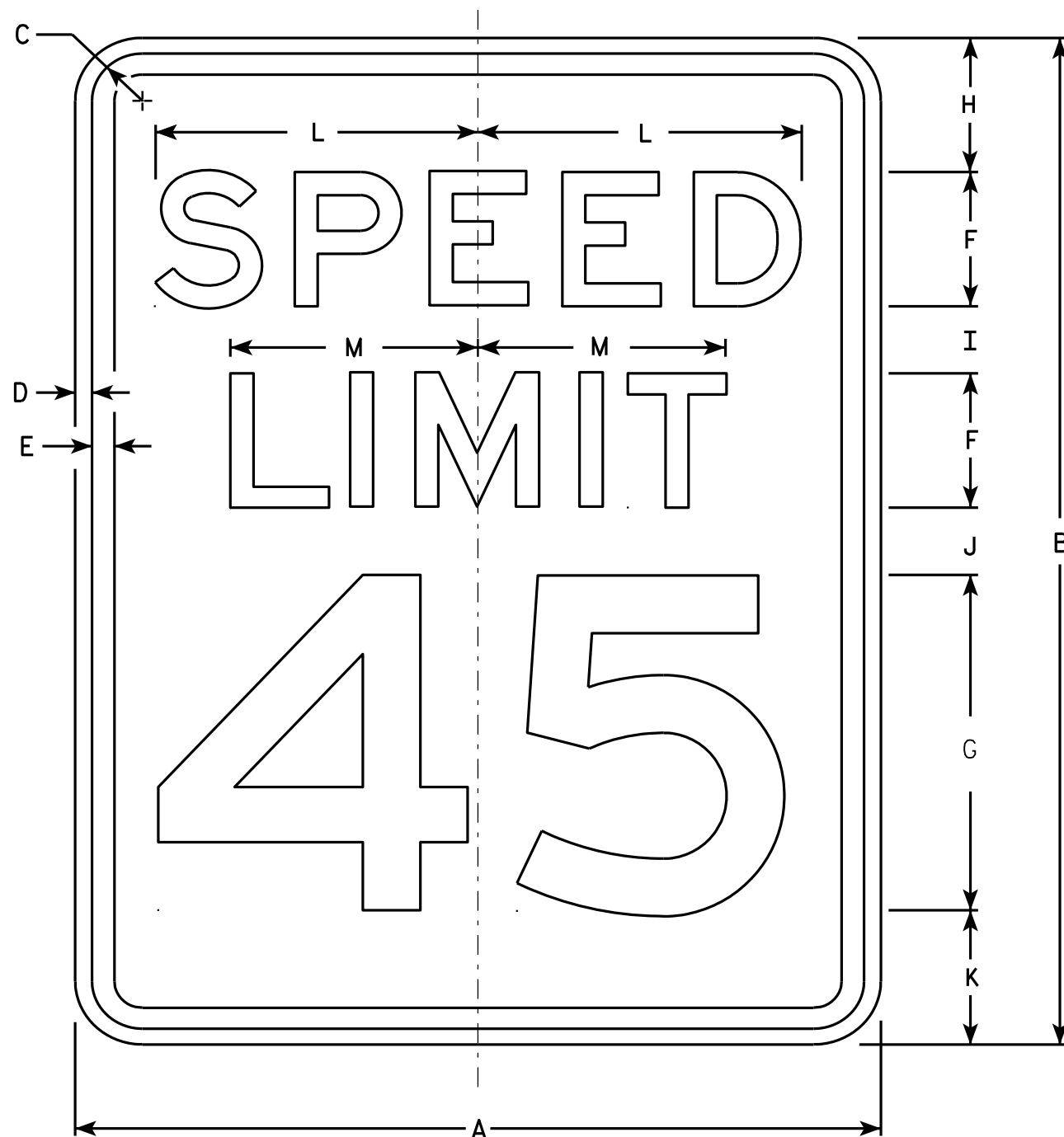
STANDARD SIGN
R1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/13/14 PLATE NO. R1-2.12

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

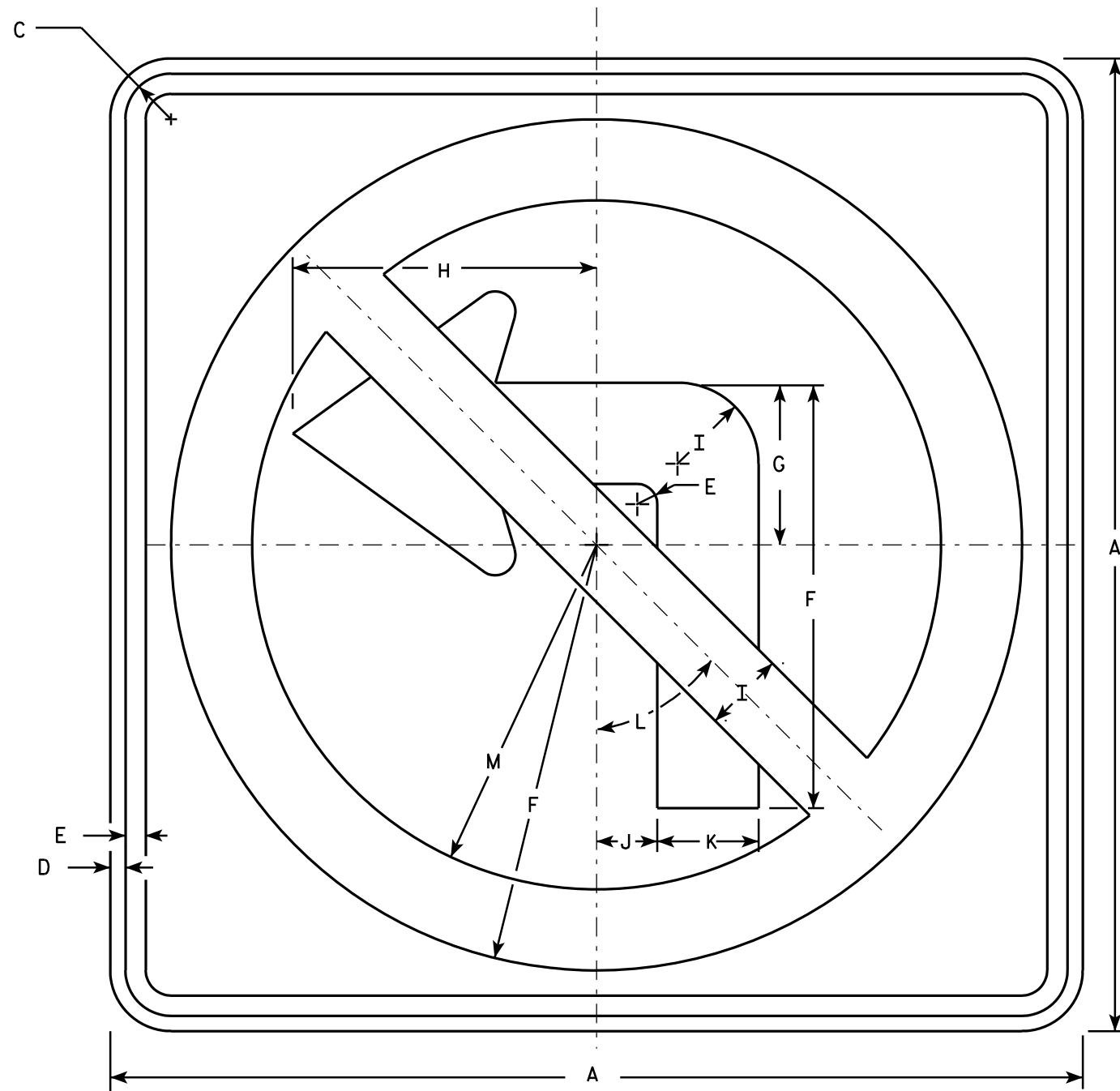
STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

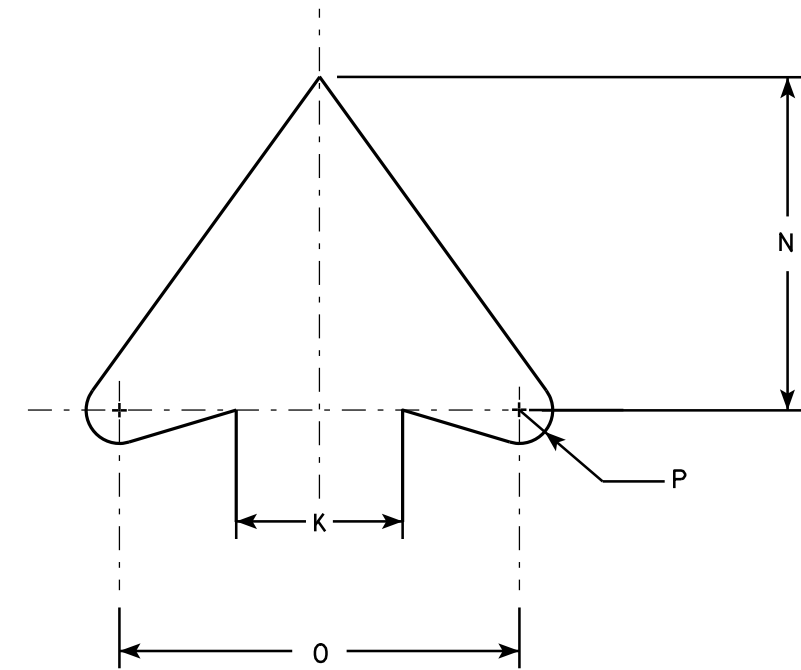
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



R3-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. Ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

STANDARD SIGN
R3-2

WISCONSIN DEPT OF TRANSPORTATION

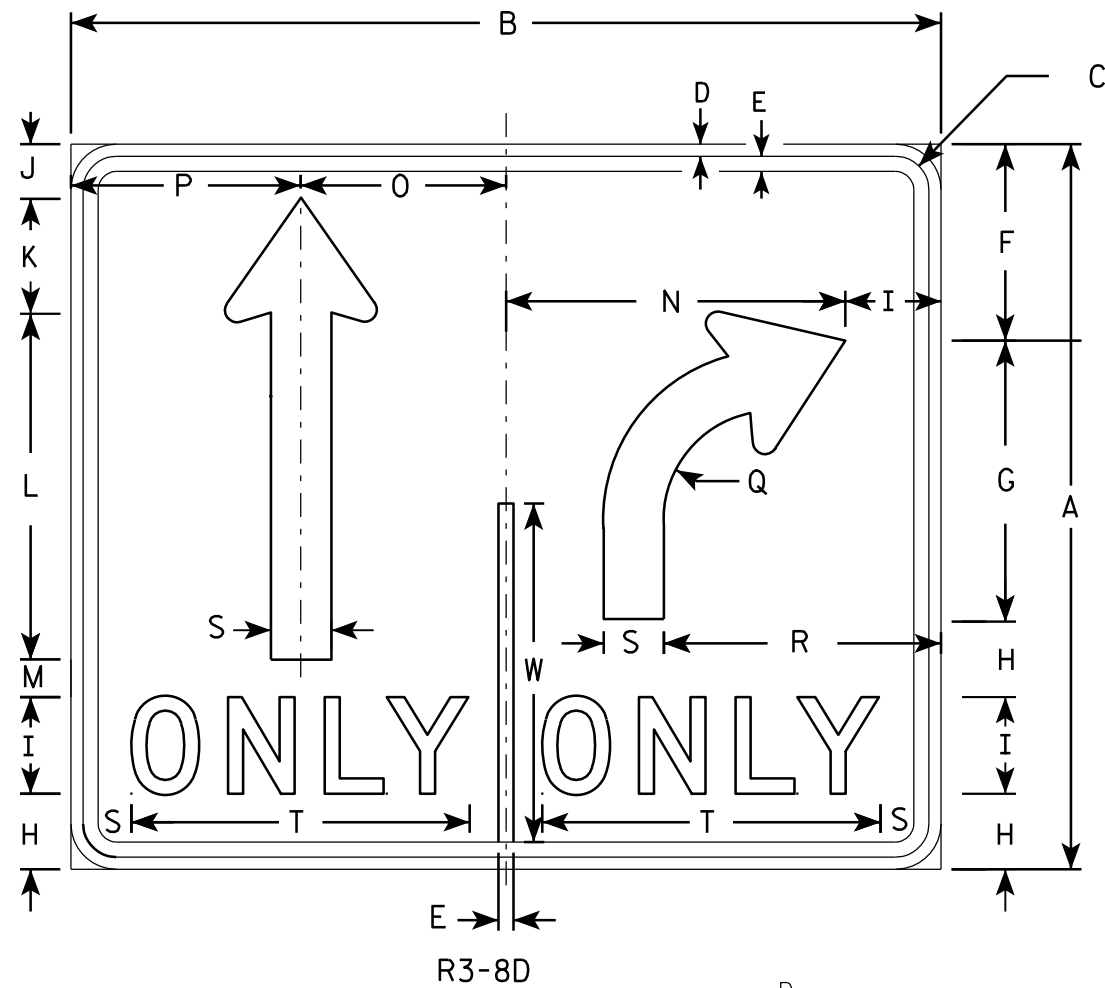
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-2.10

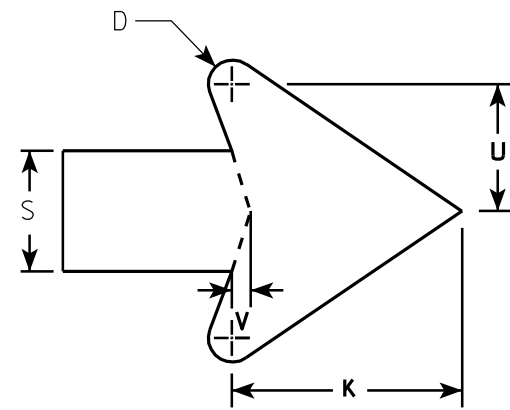
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

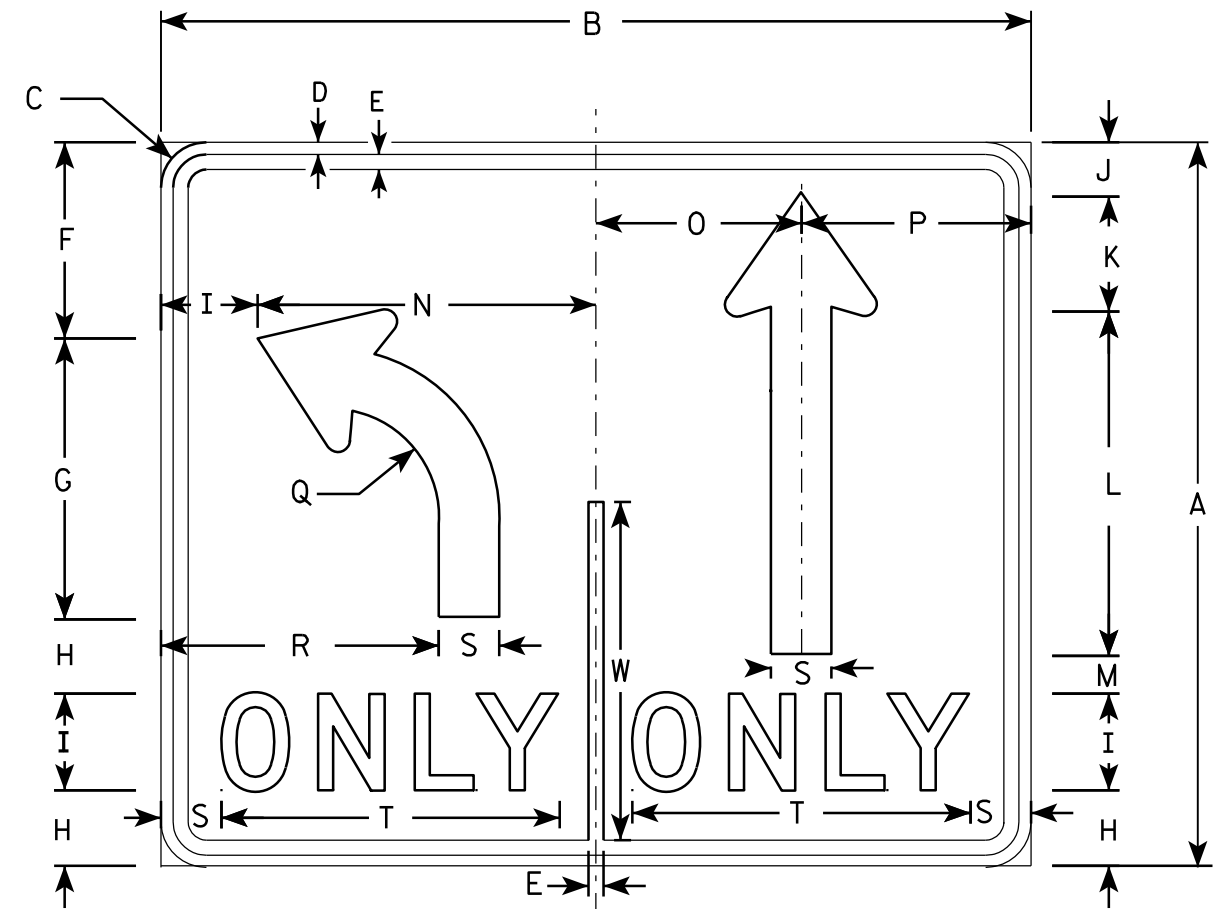
1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - WHITE
Message - BLACK
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R3-8D



ARROW DETAIL



R3-8E

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3/8	1/2	5/8	8 1/8	11 5/8	3 1/8	4	2 1/4	4 3/4	14 1/4	1 5/8	14	8 1/2	9 1/2	4 1/2	11 1/2	2 1/2	14	2 5/8	3/8	14			7.5	
2M	30	36	1 3/8	1/2	5/8	8 1/8	11 5/8	3 1/8	4	2 1/4	4 3/4	14 1/4	1 5/8	14	8 1/2	9 1/2	4 1/2	11 1/2	2 1/2	14	2 5/8	3/8	14			7.5	
3																											
4	48	54	2 1/4	3/4	1	13 1/4	18 1/2	5 1/8	6	3 1/2	7 1/8	21 1/2	4 3/4	21	12 3/4	14 1/4	7 1/4	17 1/8	3 3/4	20 5/8	4	5/8	22 3/8			18.0	
5	48	54	2 1/4	3/4	1	13 1/4	18 1/2	5 1/8	6	3 1/2	7 1/8	21 1/2	4 3/4	21	12 3/4	14 1/4	7 1/4	17 1/8	3 3/4	20 5/8	4	5/8	22 3/8			18.0	

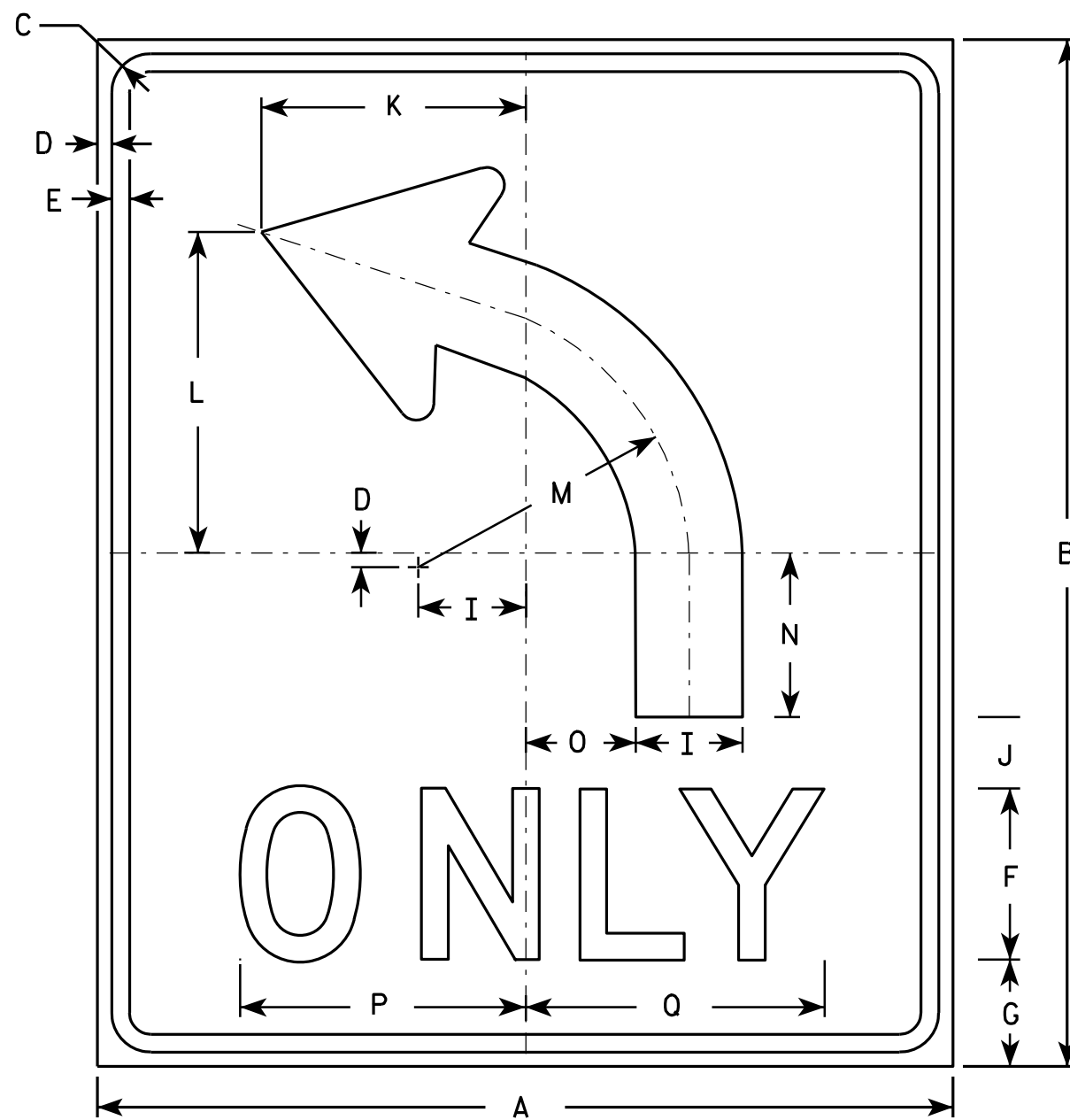
STANDARD SIGN
R3-8D & R3-8E

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/2011 PLATE NO. R3-8D.2

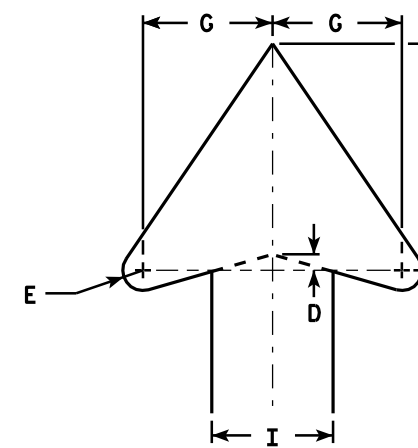
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



R3-50L

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R3-50R is the same as R3-50L except curved portion of arrow points right.



ARROW DETAIL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3/8	1/2	5/8	6	4	7	3 3/4	2 1/2	9 1/4	11 1/4	9 1/2	5 3/4	3 7/8	10	10 1/2										7.5
2M	30	36	1 3/8	1/2	5/8	6	4	7	3 3/4	2 1/2	9 1/4	11 1/4	9 1/2	5 3/4	3 7/8	10	10 1/2										7.5
3																											
4																											
5																											

STANDARD SIGN
R3-50

WISCONSIN DEPT OF TRANSPORTATION

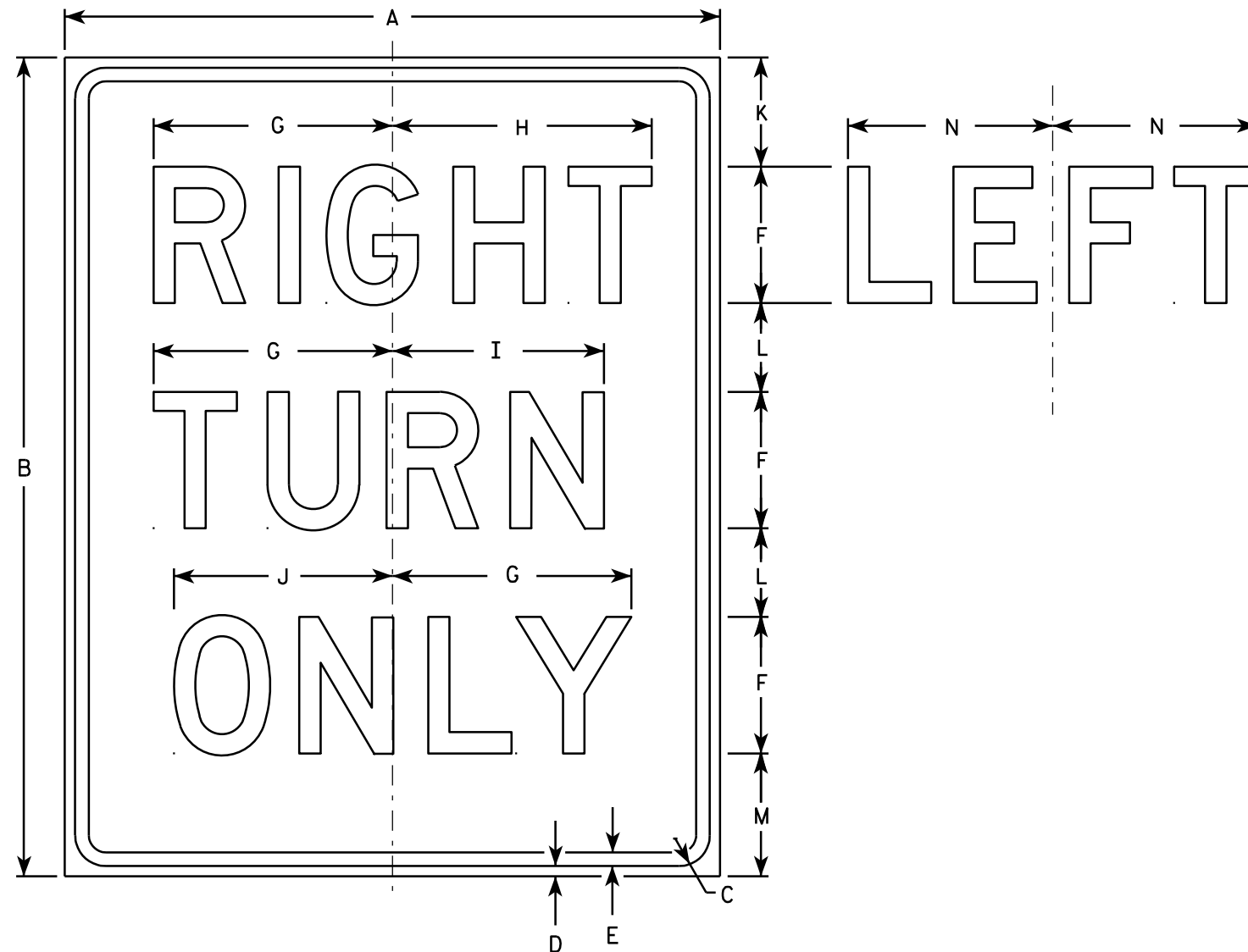
APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 3/24/2011 PLATE NO. R3-50.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R3-53L same as R3-53R except LEFT is substituted for RIGHT.



R3-53R

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	30	1 1/8	3/8	1/2	5	8 3/4	9 1/2	7 3/4	8	4	3 1/4	4 1/2	7 1/2													5.0
2M	24	30	1 1/8	3/8	1/2	5	8 3/4	9 1/2	7 3/4	8	4	3 1/4	4 1/2	7 1/2													5.0
3	24	30	1 1/8	3/8	1/2	5	8 3/4	9 1/2	7 3/4	8	4	3 1/4	4 1/2	7 1/2													5.0
4																											
5																											

STANDARD SIGN
R3-53

WISCONSIN DEPT OF TRANSPORTATION

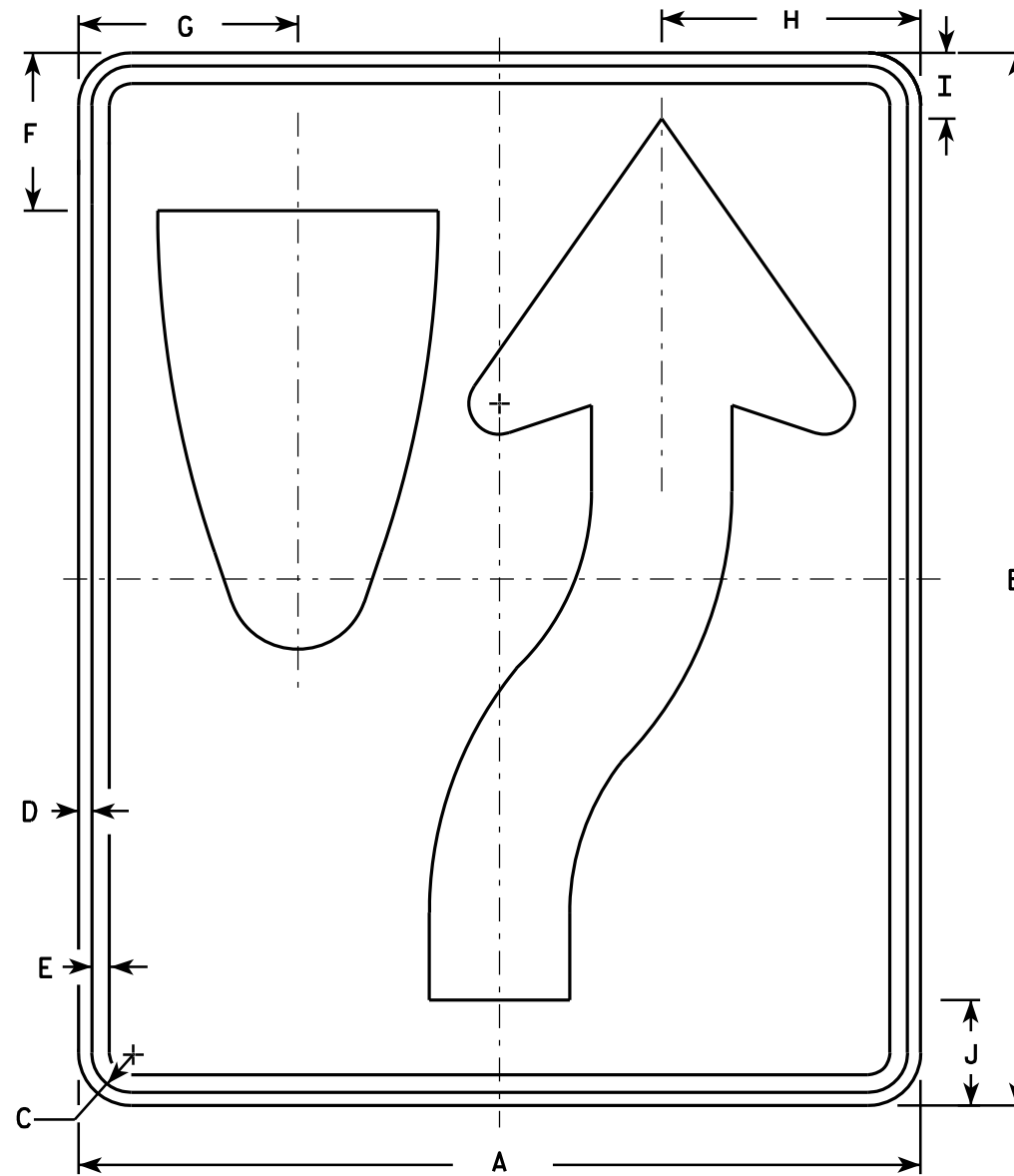
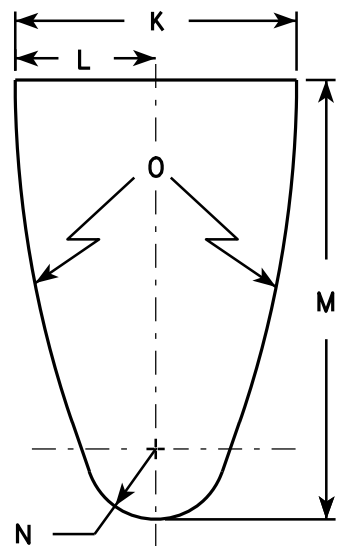
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/24/2011 PLATE NO. R3-53.8

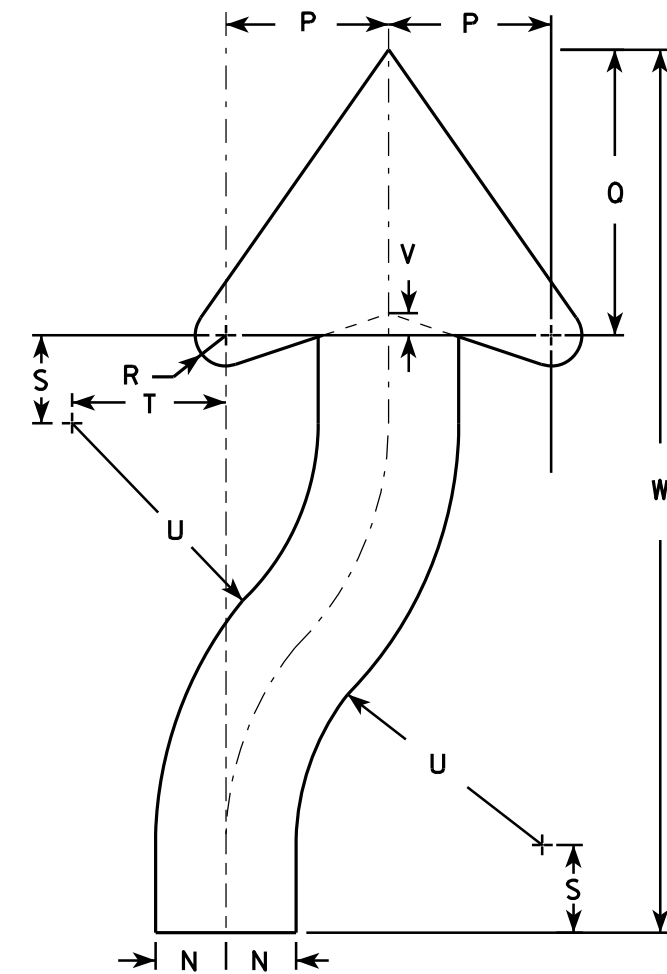
PROJECT NO:	HWY:	COUNTY:	SHEET NO: E
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NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
2. Color:
Background - White
Message - Black
3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
4. R4-8 is the same as R4-7 except Legend is reversed.



R4-7



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3 3/8	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5/8	1 7/8	3 1/4	6 3/4	1/2	20 3/8				3.0
2S	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2M	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 5/8	5	8 3/4	18	1 1/4	50 1/4				20.0

STANDARD SIGN
R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

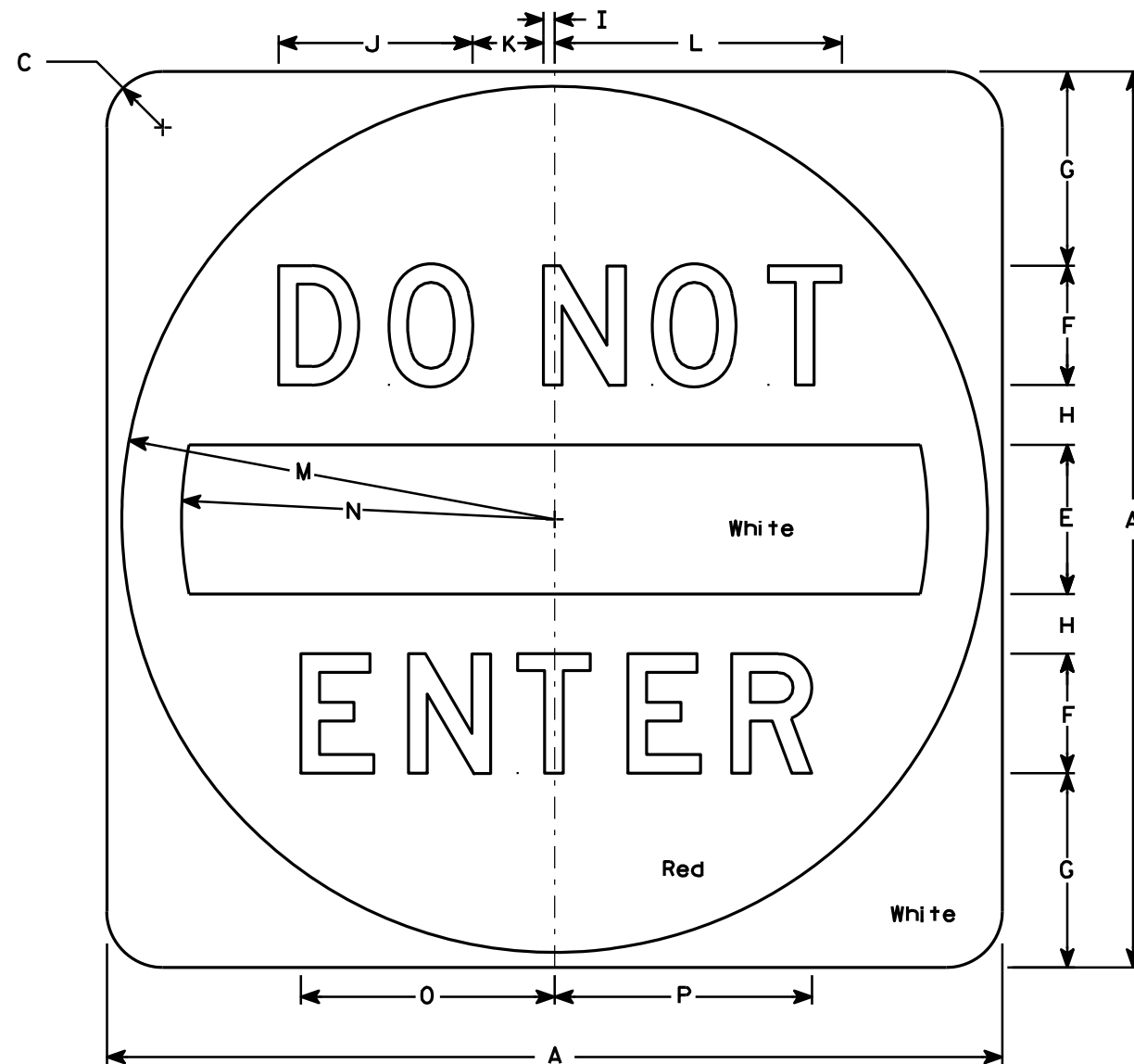
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-7.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - See detail
Message - White - Type H Reflective
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but when base material is metal, the corners shall be rounded.



R5-1

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 7/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 5/8	14 1/2	12 1/2	8 1/2	8 5/8											6.26
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 5/8	14 1/2	23 1/2	20	12 3/4	12 7/8											16.0

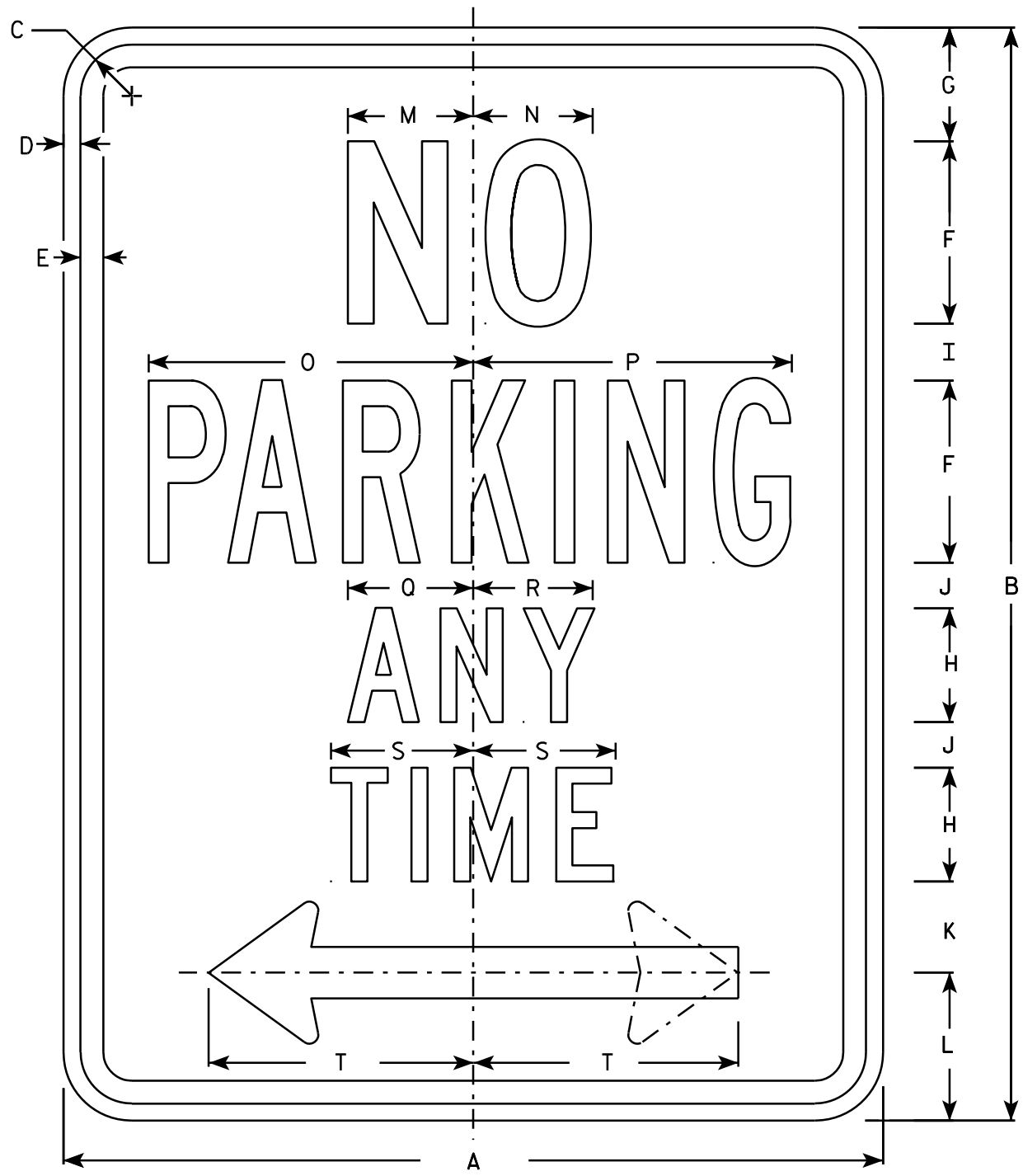
STANDARD SIGN
R5-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 12/17/10 PLATE NO. R5-1.15

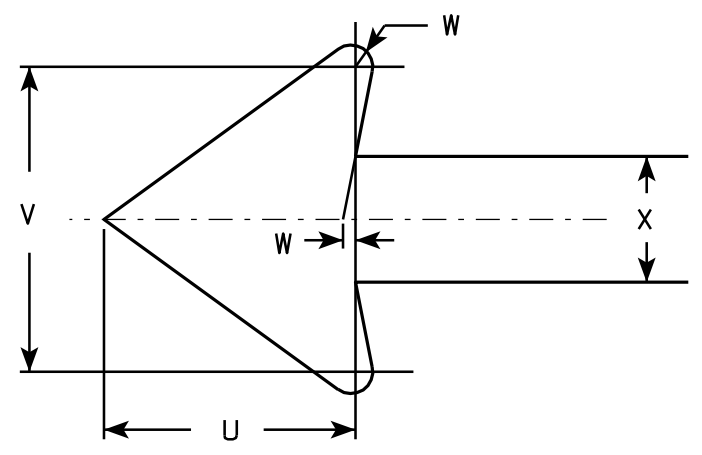
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



R7-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)
R7-1L (left arrow)
R7-1R (right arrow)



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

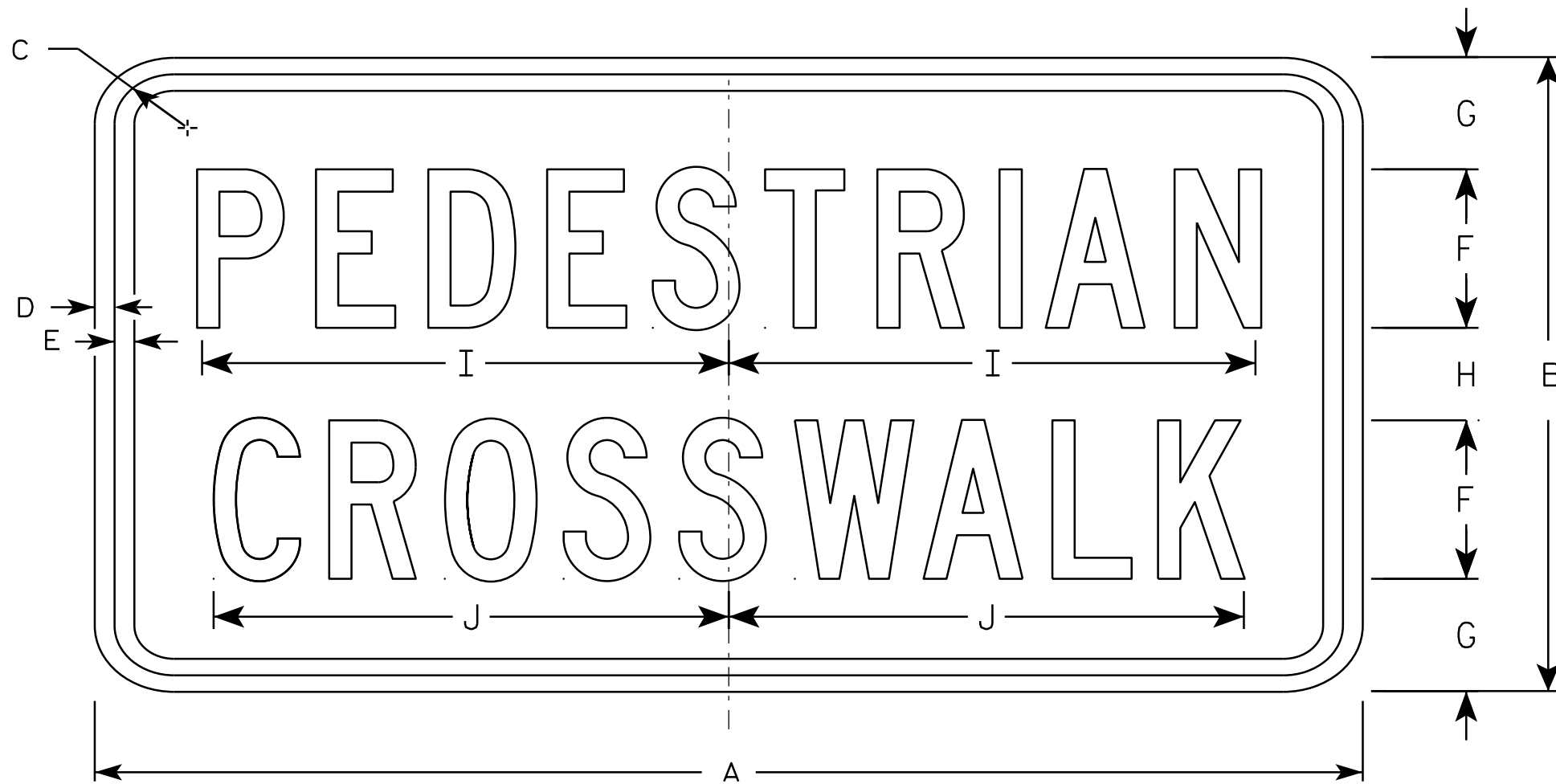
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R9-8

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	9 3/4																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	9 3/4																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	13	13																	3.75
4																											
5																											

STANDARD SIGN
R9-8

WISCONSIN DEPT OF TRANSPORTATION

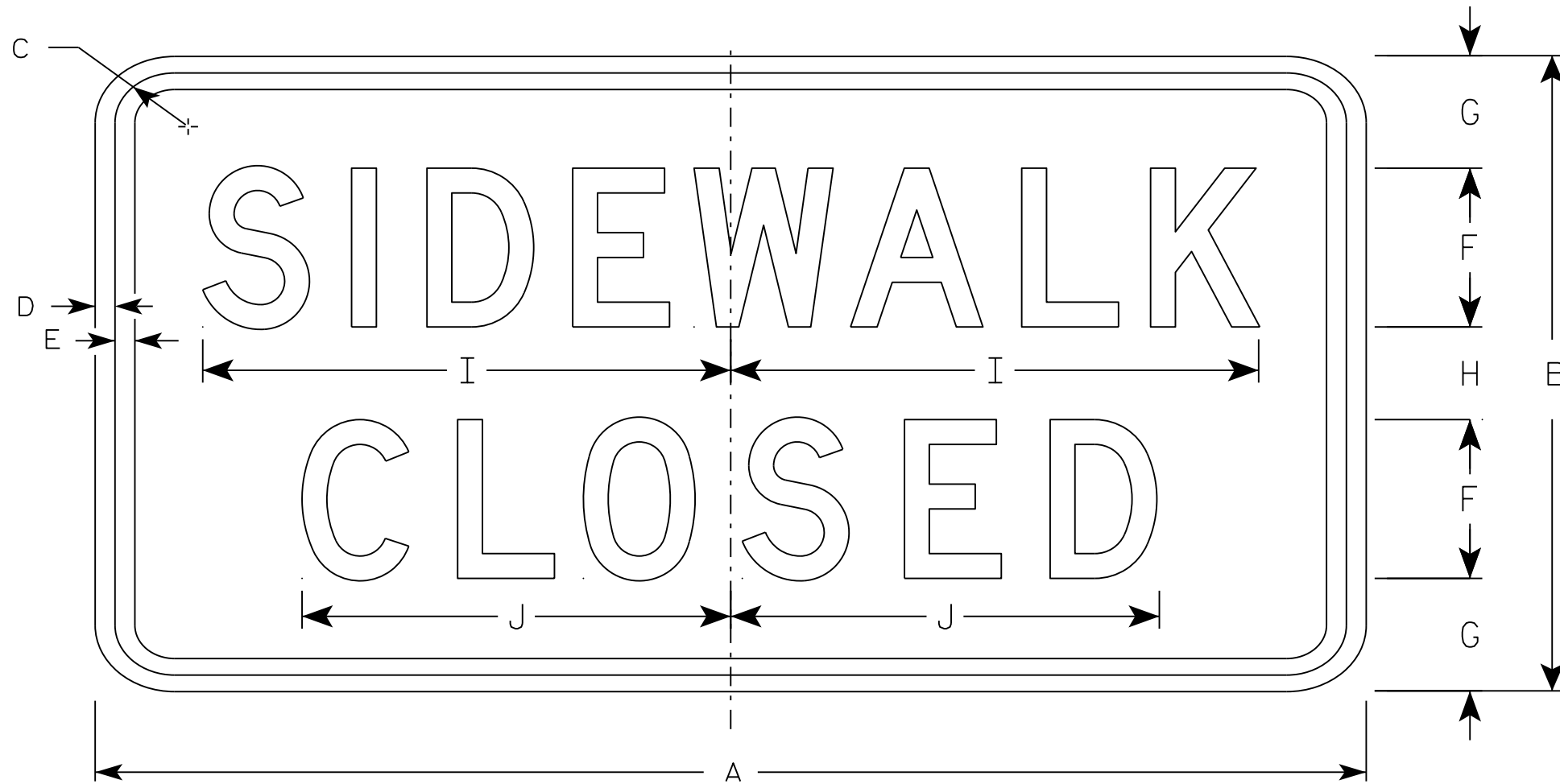
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/17/16 PLATE NO. R9-8.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

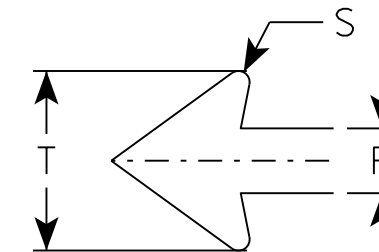
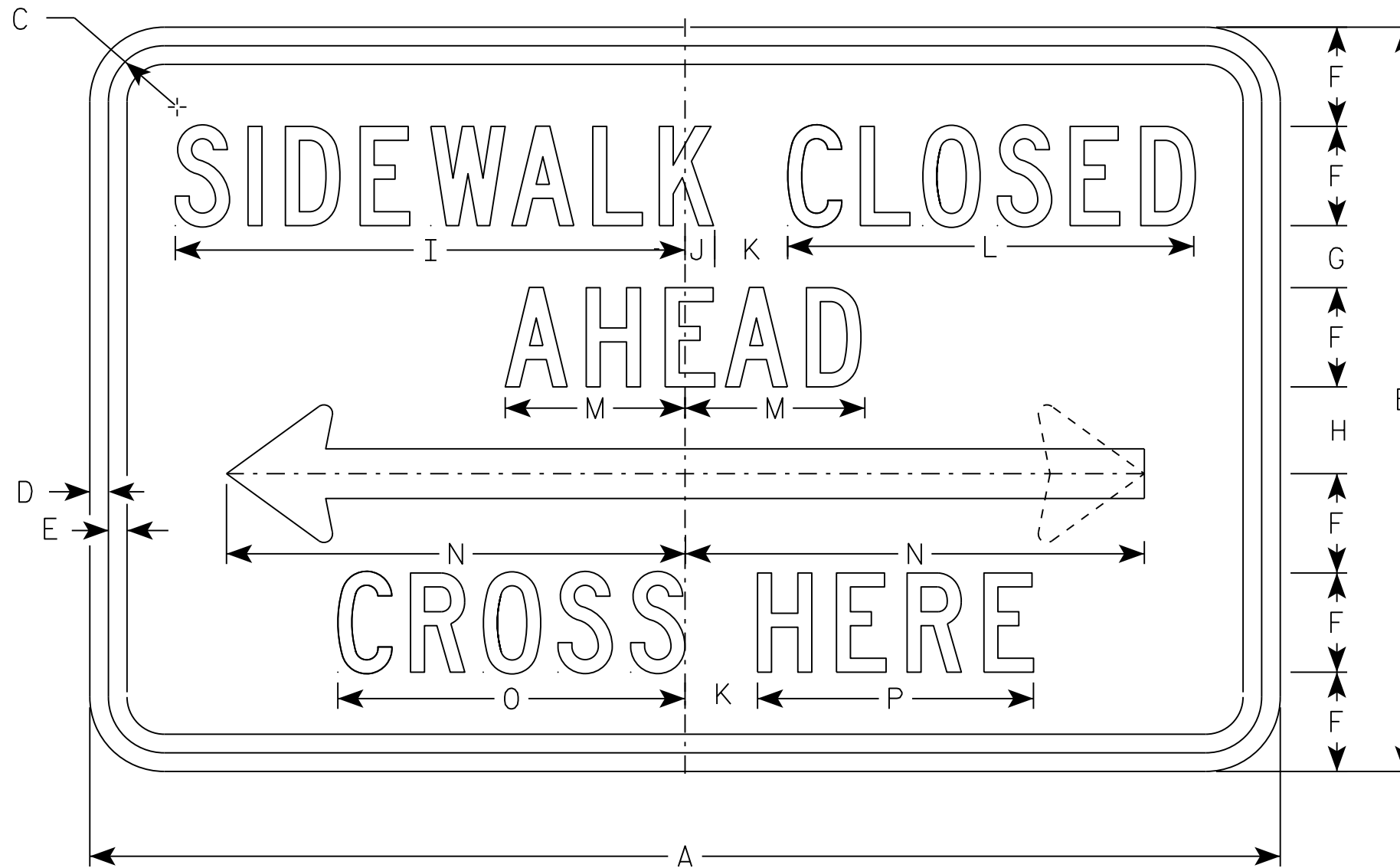
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/11/16 PLATE NO. R9-9.6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C except Size 1 is Series D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-11

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 5/8	3 1/2	9 1/4	6 5/8	5 1/8		1	1/8	2 3/4						2.0	
2M	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 5/8	3 1/2	9 1/4	6 5/8	5 1/8		1	1/8	2 3/4						2.0	
3	30	15	1 1/8	3/8	1/2	2	1 1/2	1 1/2	13	3/4	2	10 1/4	4 5/8	12 3/8	8 7/8	6 7/8		1 1/4	1/4	3 5/8						3.125	
4																											
5																											

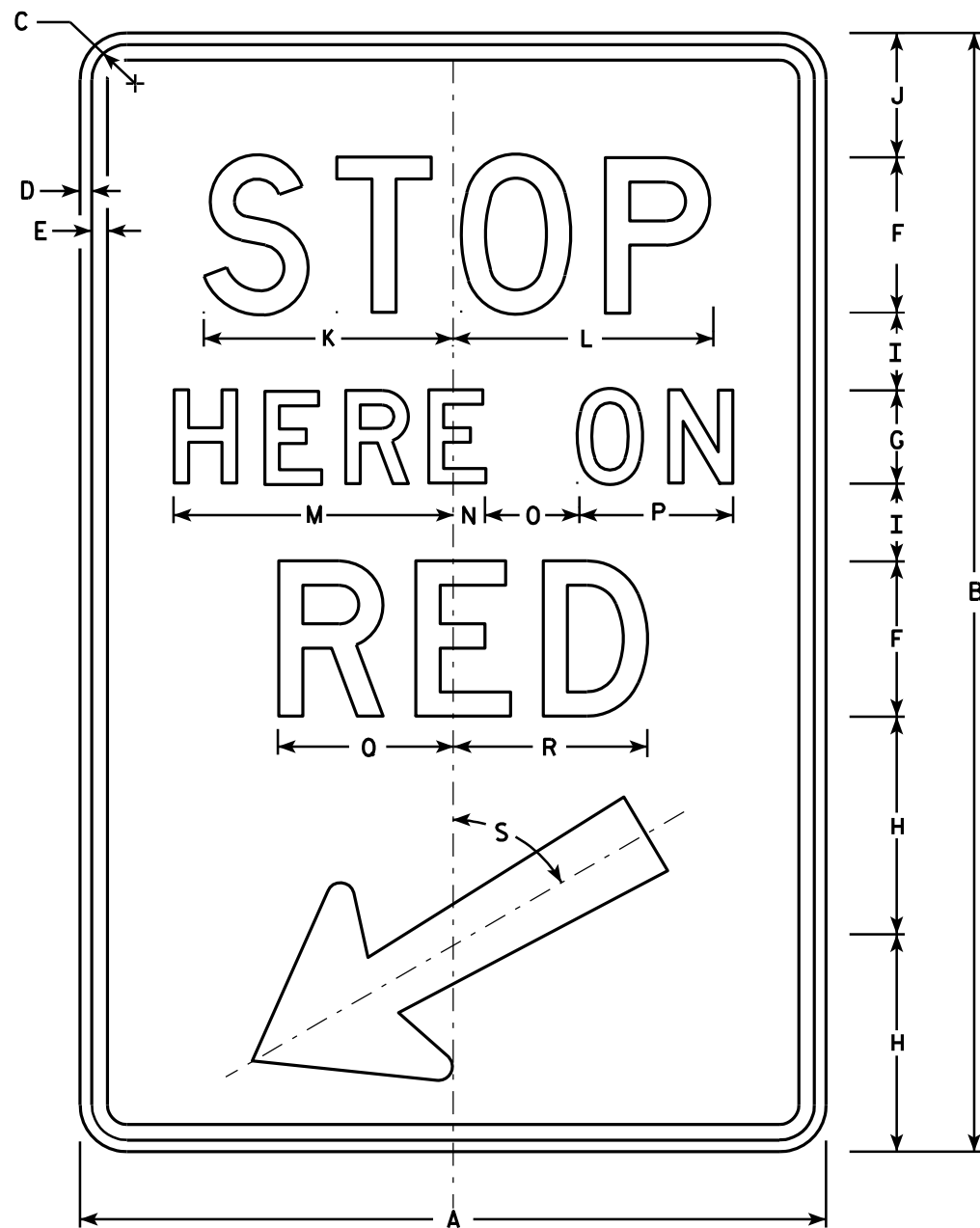
STANDARD SIGN
R9-11

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/29/16 PLATE NO. R9-11.3

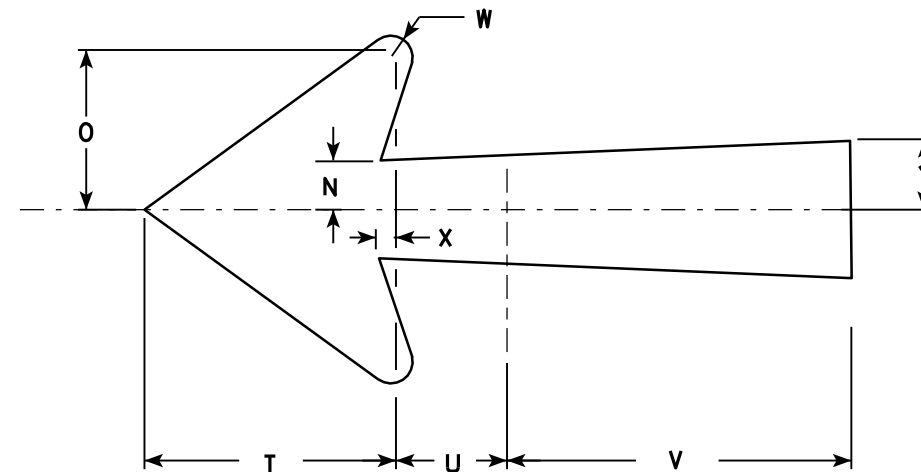
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



R10-6

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

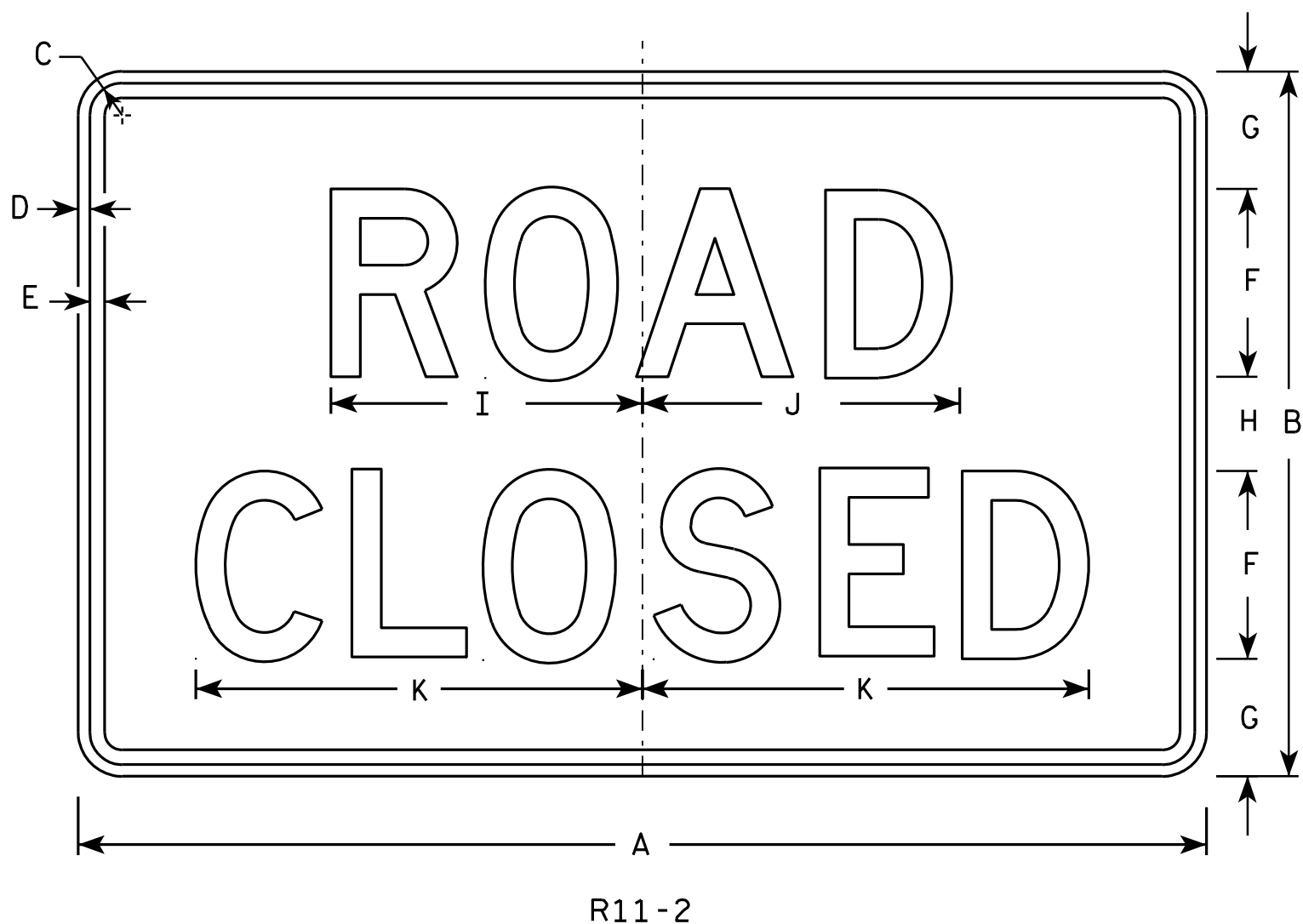
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/8	3/8	1/2	5	3	7	2 1/2	4	8	8 3/8	9	1	3	5	5 5/8	6 1/4	60°	5 1/4	2 1/4	7 1/8	1/2	3/8	1 3/8	6.0	
2M	24	36	1 1/8	3/8	1/2	5	3	7	2 1/2	4	8	8 3/8	9	1	3	5	5 5/8	6 1/4	60°	5 1/4	2 1/4	7 1/8	1/2	3/8	1 3/8	6.0	
3																											
4																											
5																											

STANDARD SIGN
R10-6

WISCONSIN DEPT OF TRANSPORTATION

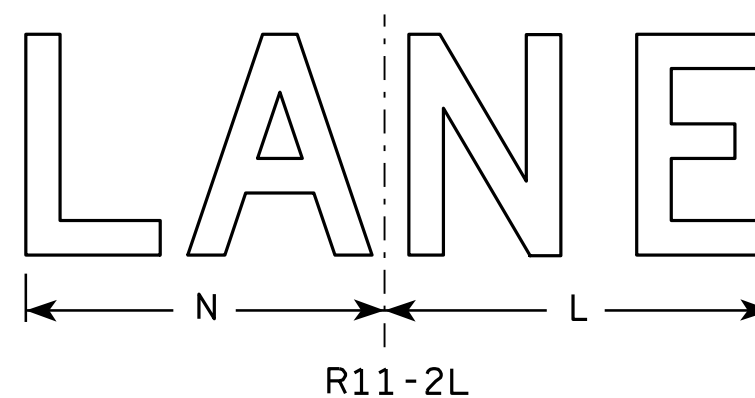
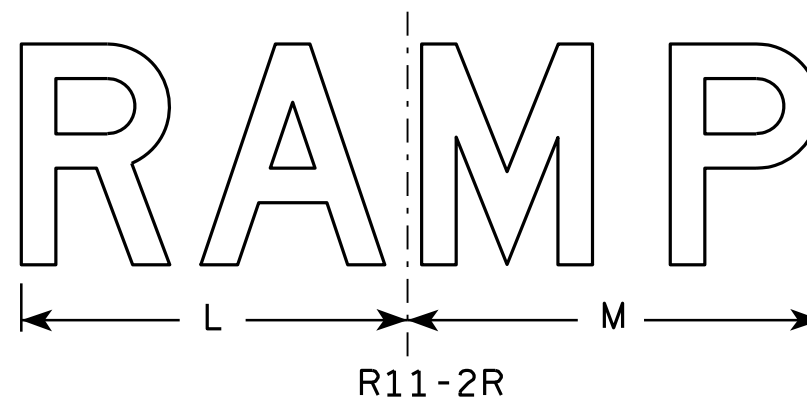
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/5/11 PLATE NO. R10-6.6



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

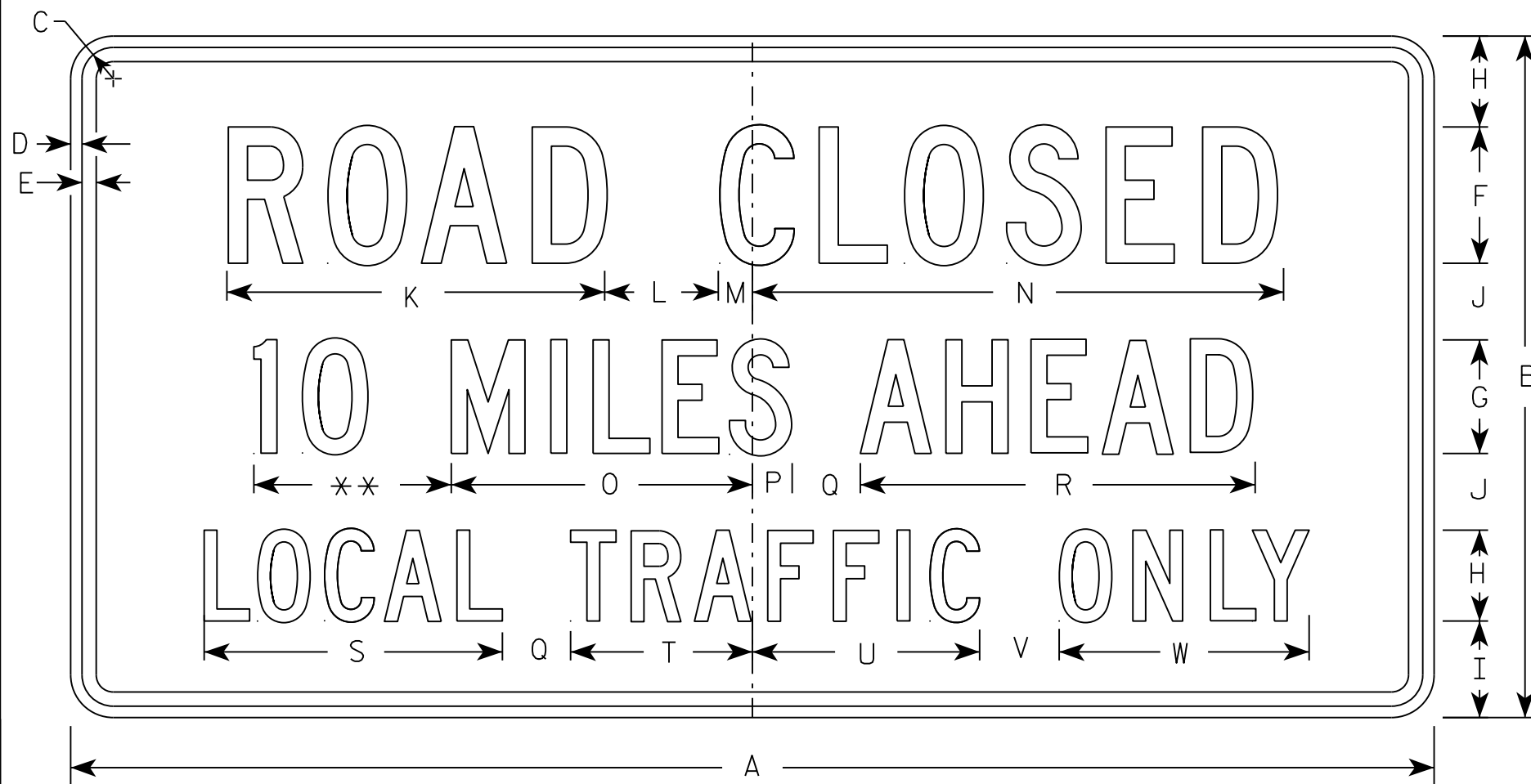
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-2.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

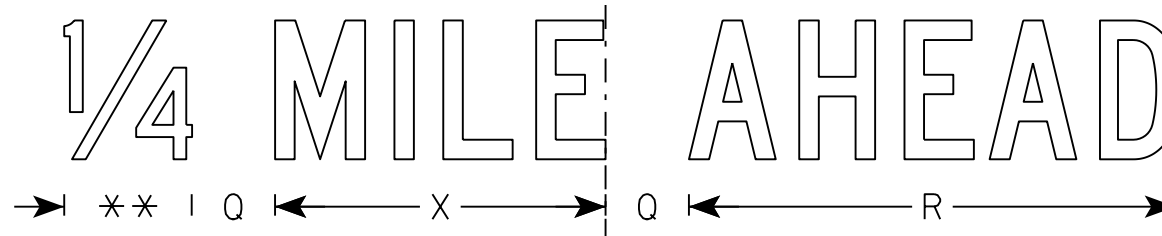
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 3/8	1/2	5/8	4	3	2 1/2	2	2	11 1/8	3	1 1/8	15 1/4	8	1 1/2	2	10 3/4	8 3/8	4 3/4	6 1/2	2	6 3/4	7 1/8		4.5	
2S	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	16 5/8	5	1 1/2	23	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11	11 7/8		12.5	
2M	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	16 5/8	5	1 1/2	23	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11	11 7/8		12.5	
3																											
4																											
5																											

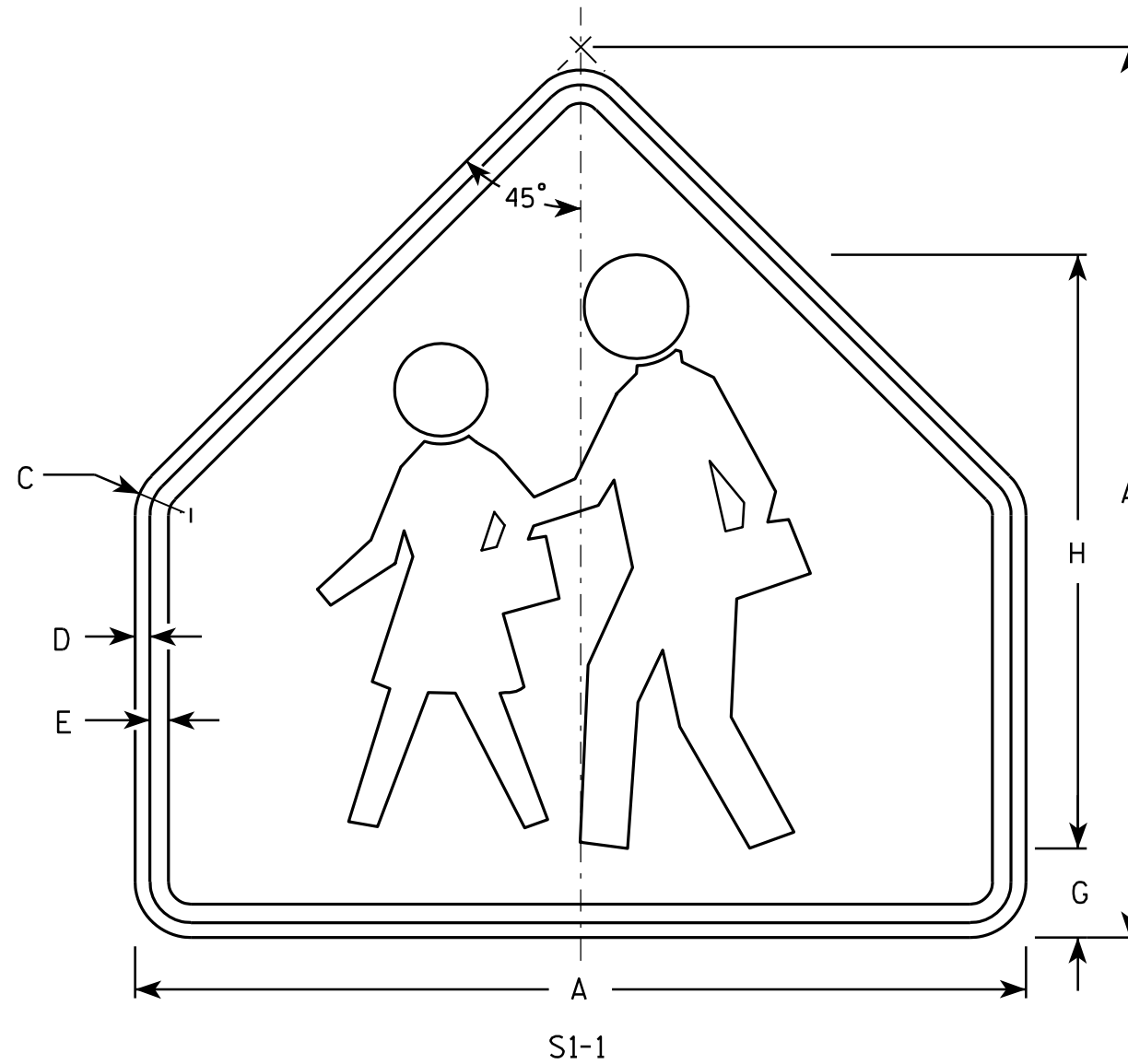
STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/15/17 PLATE NO. R11-3.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow-Green
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
3	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
4	48		2 1/4	3/4	1		4 3/4	32																			12
5																											

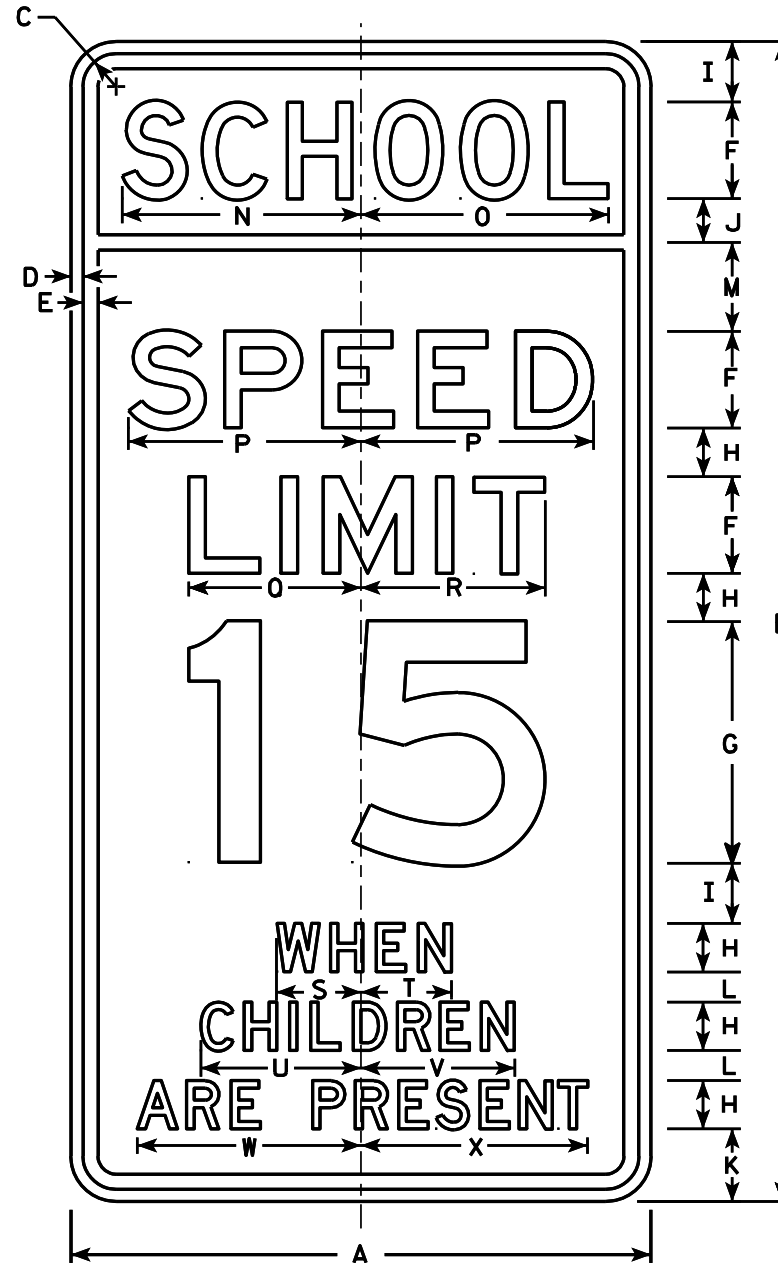
STANDARD SIGN
S1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/30/05 PLATE NO. S1-1.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



S4-51

NOTES

1. Sign is Type II - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. (See note 5).
2. Color:
Background - See note 5
Message - Black
3. Message Series - See note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Top panel (SCHOOL) background - Yellow Green -Type F Reflective. Lower panel background - White -Type H Reflective.
6. From top to bottom:
Lines 1, 5, 6 & 7 are series D
Lines 2, 3 & 4 are series E
7. Line 4 substitute appropriate numerals and adjust spacing to achieve proper balance.

7

7

Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 1200 mm
3	900 mm X 1800 mm
4	
5	

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	24	48	1 3/8	1/2	5/8	4	10	2	2 1/2	1 3/4	3	1 1/4	3 3/4	9 7/8	10 1/4	9 5/8	7 1/8	7 5/8	3 1/2	3 3/8	6 5/8	6 3/8	9 1/4	9 3/8		8.00	0.72	
3	36	72	2 1/4	3/4	1	6	15	3	3 3/4	2 3/4	4 1/2	1 7/8	5 1/2	15	15 1/4	14 1/2	11 1/4	11 1/2	5 1/2	5 3/4	10	9 3/4	14	14 1/8		18.00	1.62	
4																												
5																												

STANDARD SIGN
S4-51

WISCONSIN DEPT OF TRANSPORTATION

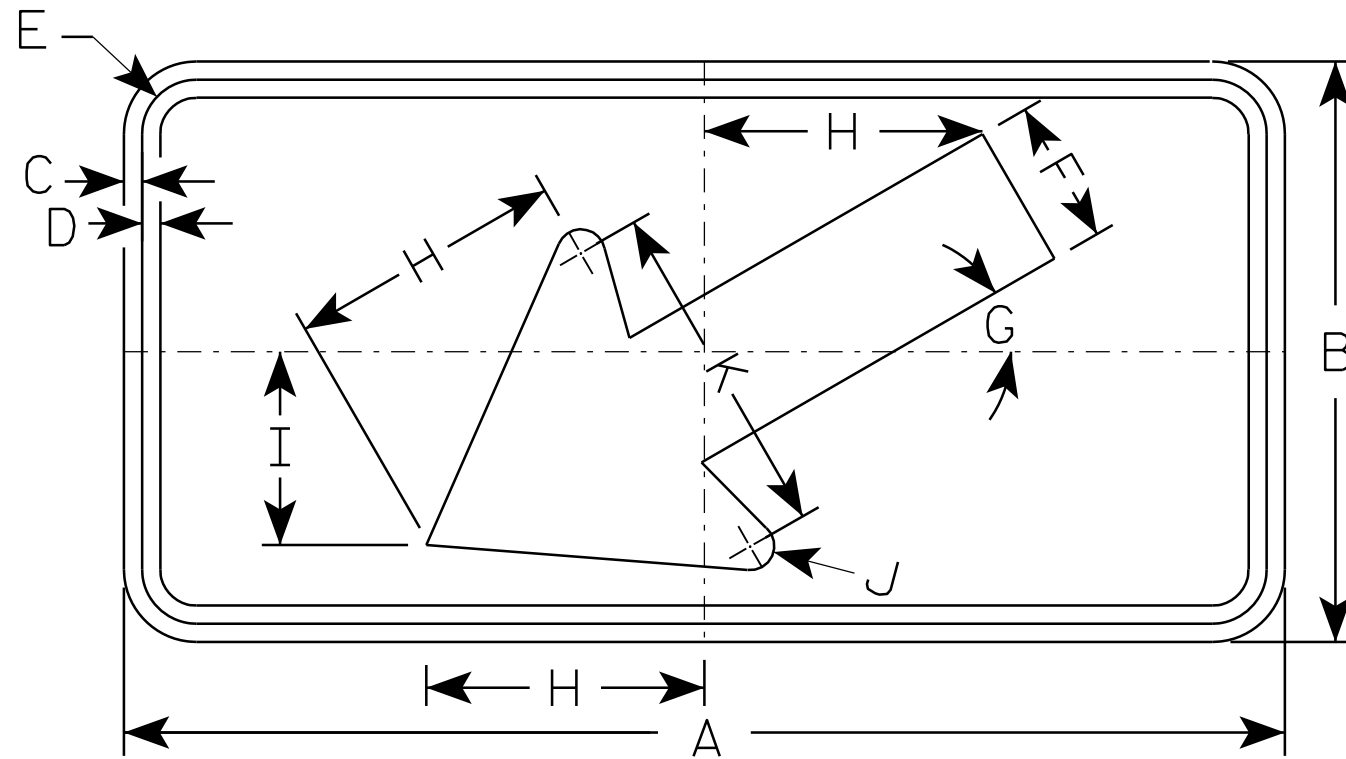
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/26/10 PLATE NO. S4-51.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow-Green
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. S16-7R are the same as S16-7L except the arrow is reversed along the vertical centerline.



S16-7L

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	12	3/8	3/8	1 1/8	3	30°	5 3/4	4	1/2	7																2.0
2S	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
2M	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
3	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
4	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0
5																											

STANDARD SIGN
S16-7

WISCONSIN DEPT OF TRANSPORTATION

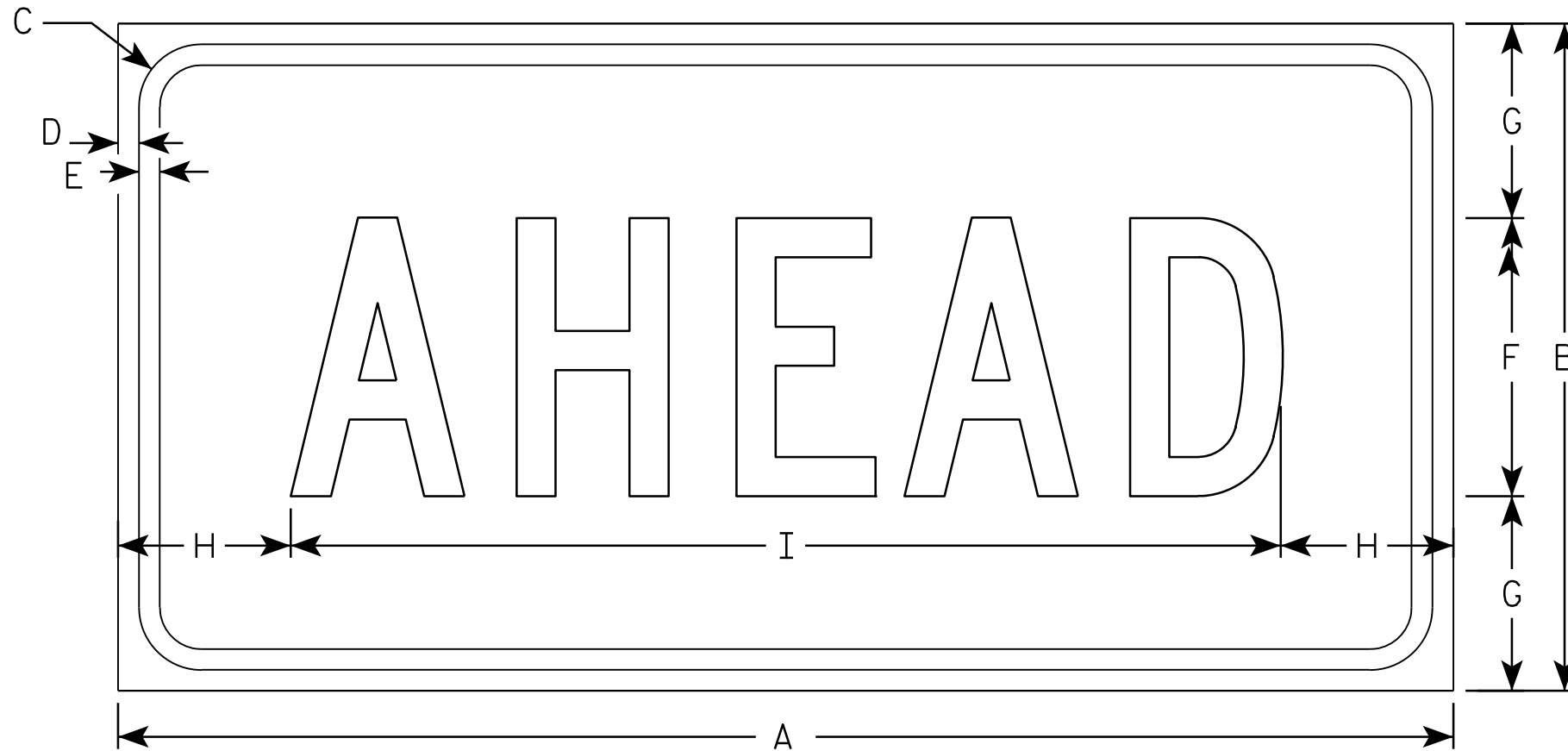
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 7/22/13 PLATE NO. S16-7.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow-Green
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



S16-9P

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	12	1 1/8	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
2S	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
2M	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/8	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

STANDARD SIGN

S16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/22/13 PLATE NO. S16-9P.1

PROJECT NO:

HWY:

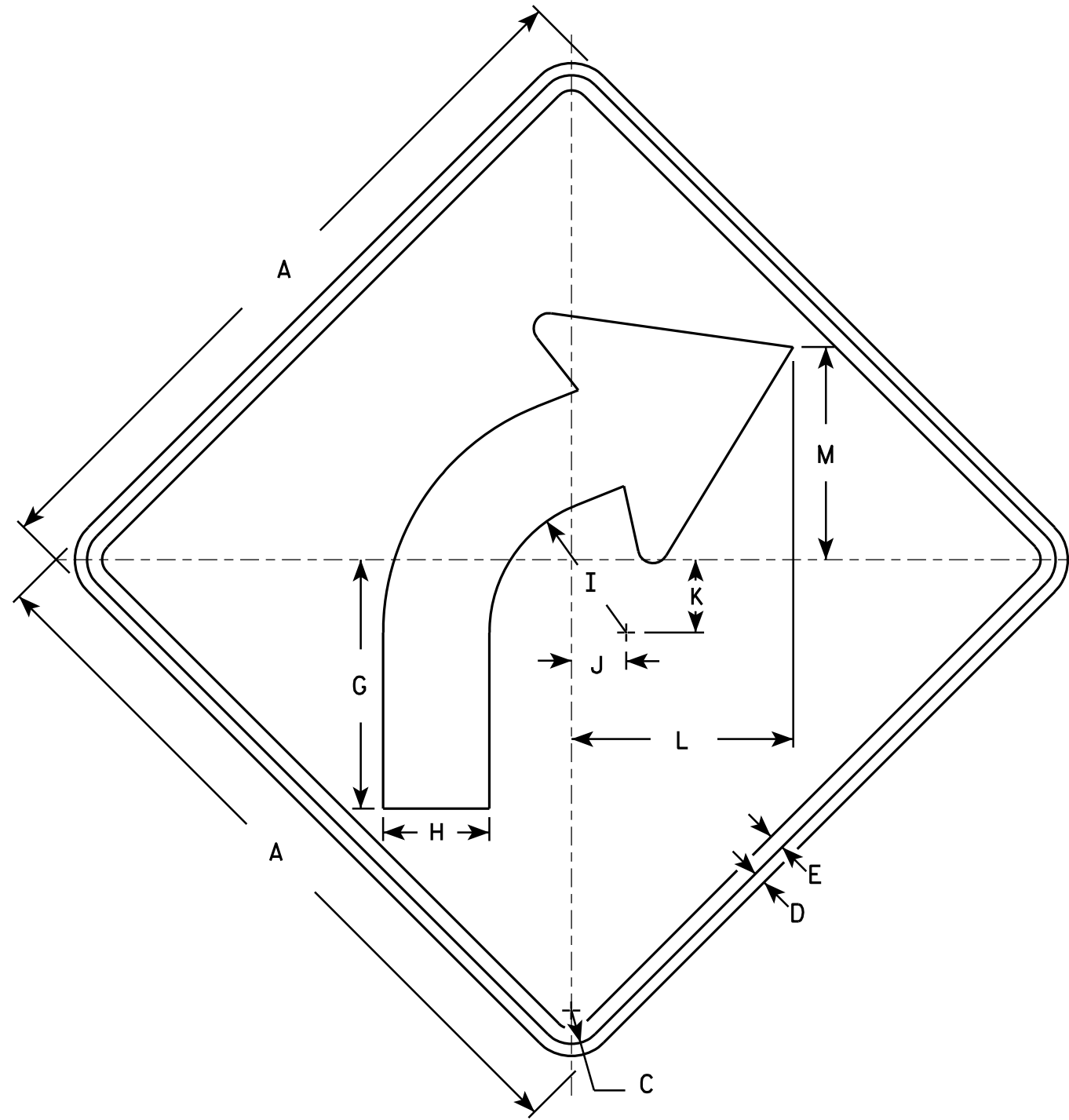
COUNTY:

SHEET NO:

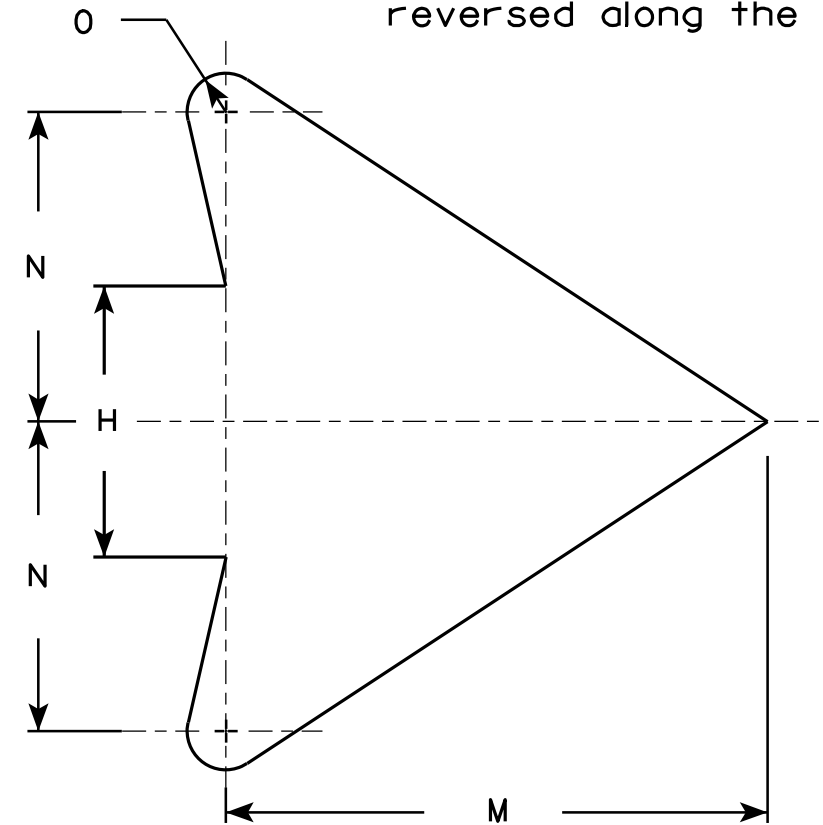
E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

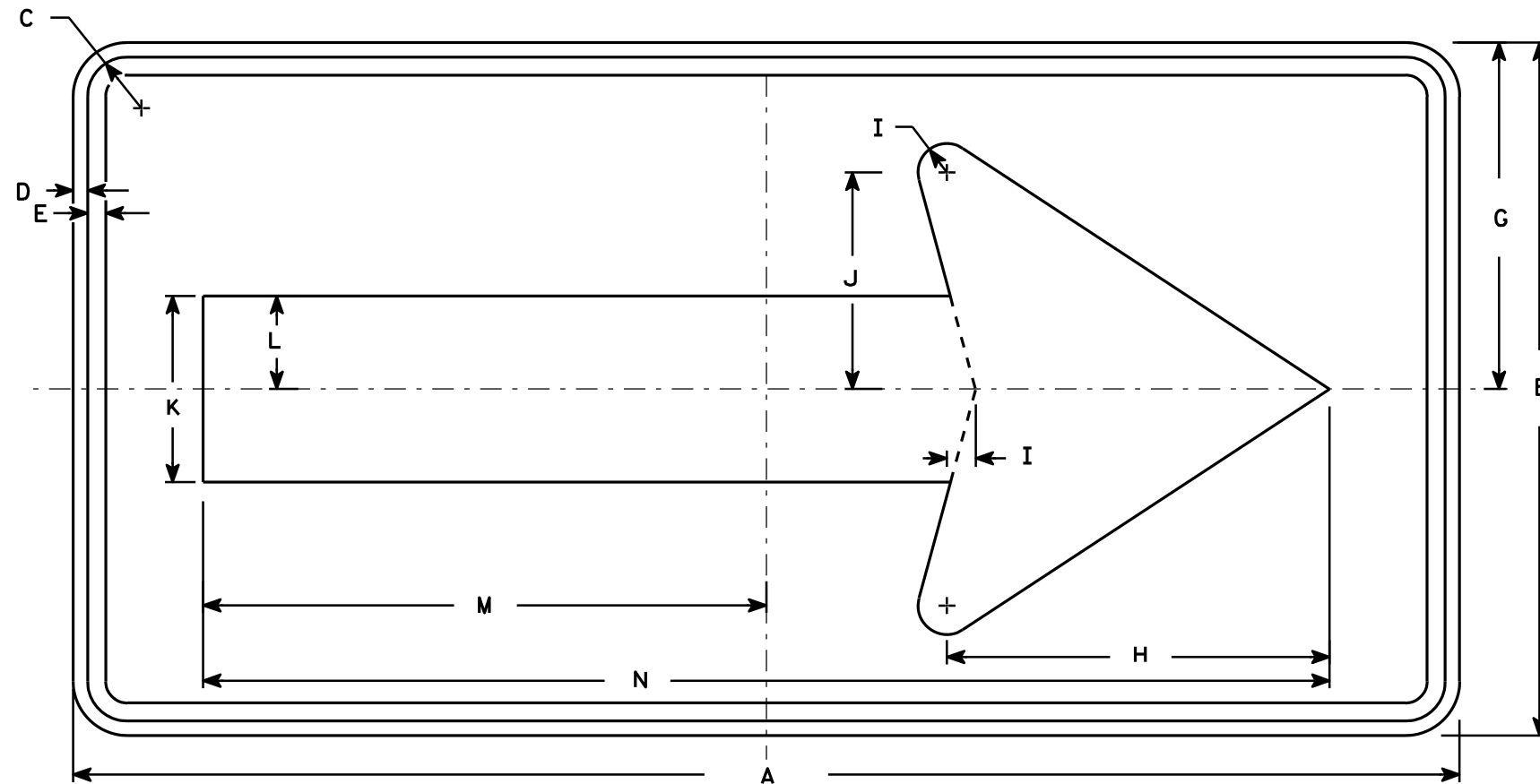
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

**STANDARD SIGN
W1-2**

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 5/15/12 PLATE NO. W1-2.10

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W1-6

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	3/8		9	10	3/4	5 5/8	4 3/4	2 3/8	14 5/8	29 1/4													4.5
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	96	48	2 1/4	3/4	1		24	26 1/2	2	15	13	6 1/2	39	78													32.0

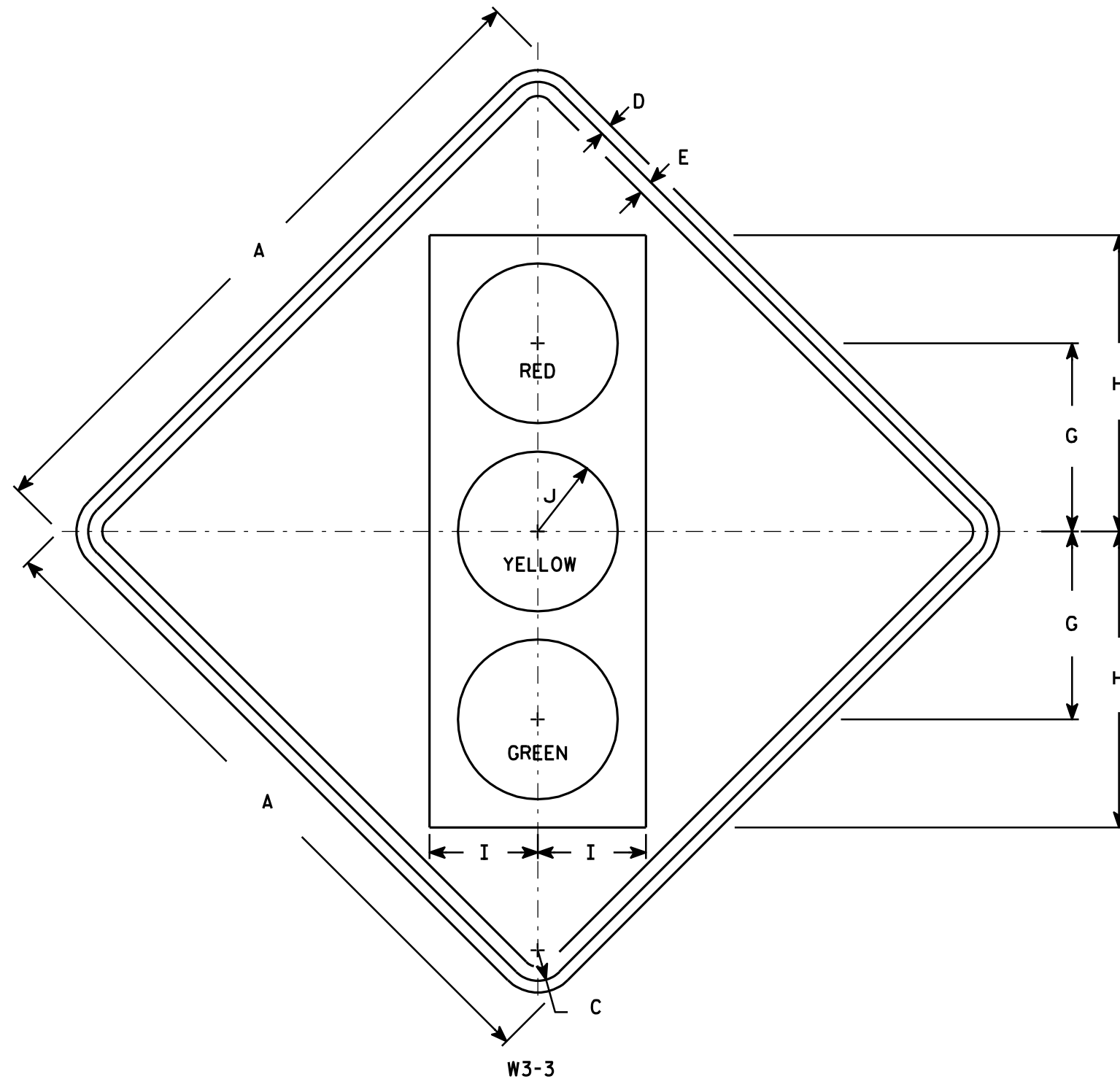
STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-6.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - See Note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Symbol and border are non-reflective black.
Top circle - Type H ReflectORIZED Red
Center circle - Same as background
Bottom circle - Type H ReflectORIZED Green

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		8 3/4	13 3/4	5	3 3/4																	6.25
2S	36		1 5/8	5/8	3/4		10	15 3/4	5 3/4	4 1/4																	9.0
2M	36		1 5/8	5/8	3/4		10	15 3/4	5 3/4	4 1/4																	9.0
3	36		1 5/8	5/8	3/4		10	15 3/4	5 3/4	4 1/4																	9.0
4	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0
5	48		2 1/4	3/4	1		12 1/2	20	7 1/2	5																	16.0

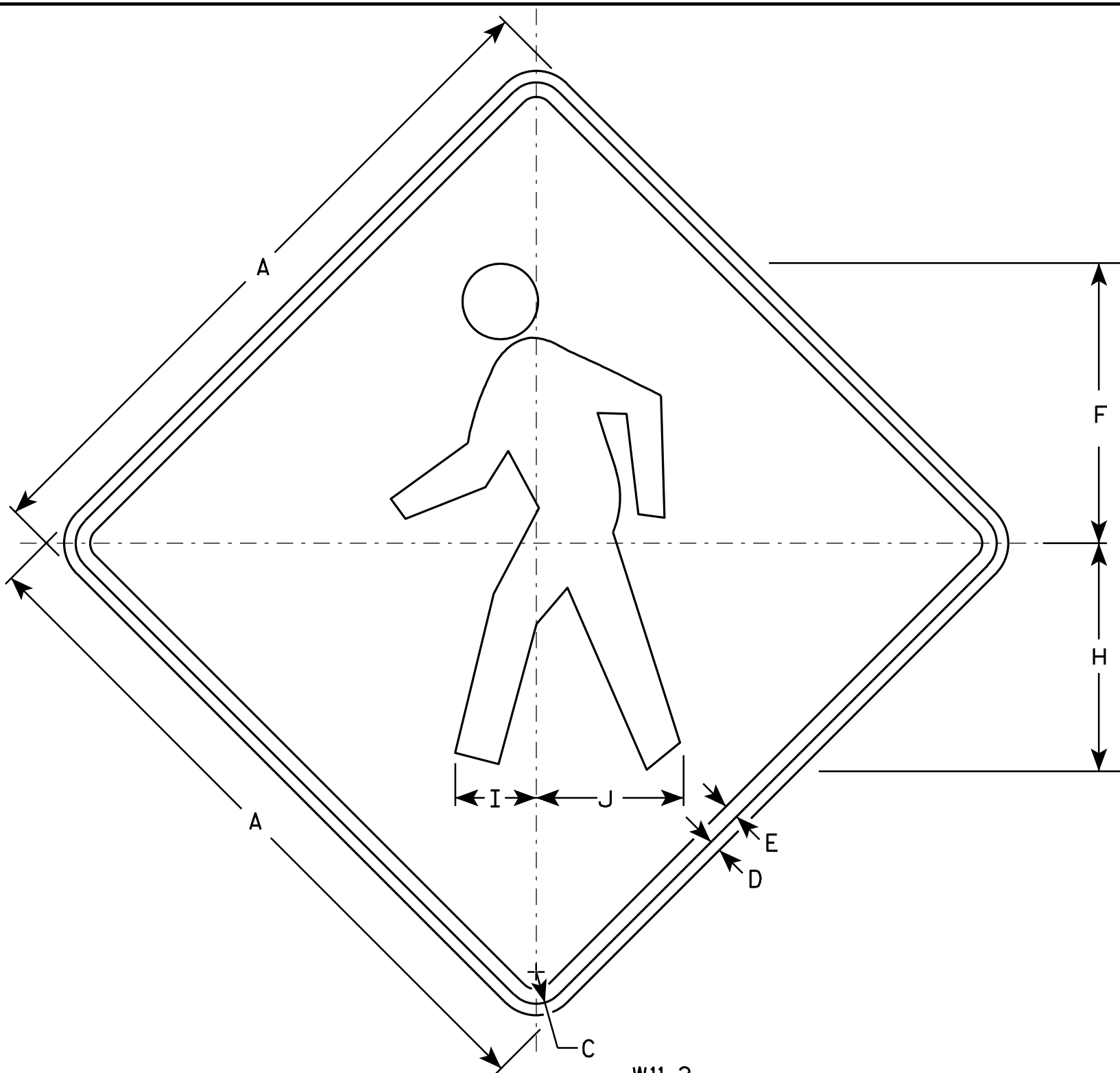
STANDARD SIGN
W3-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W3-3.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

W11-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 3/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

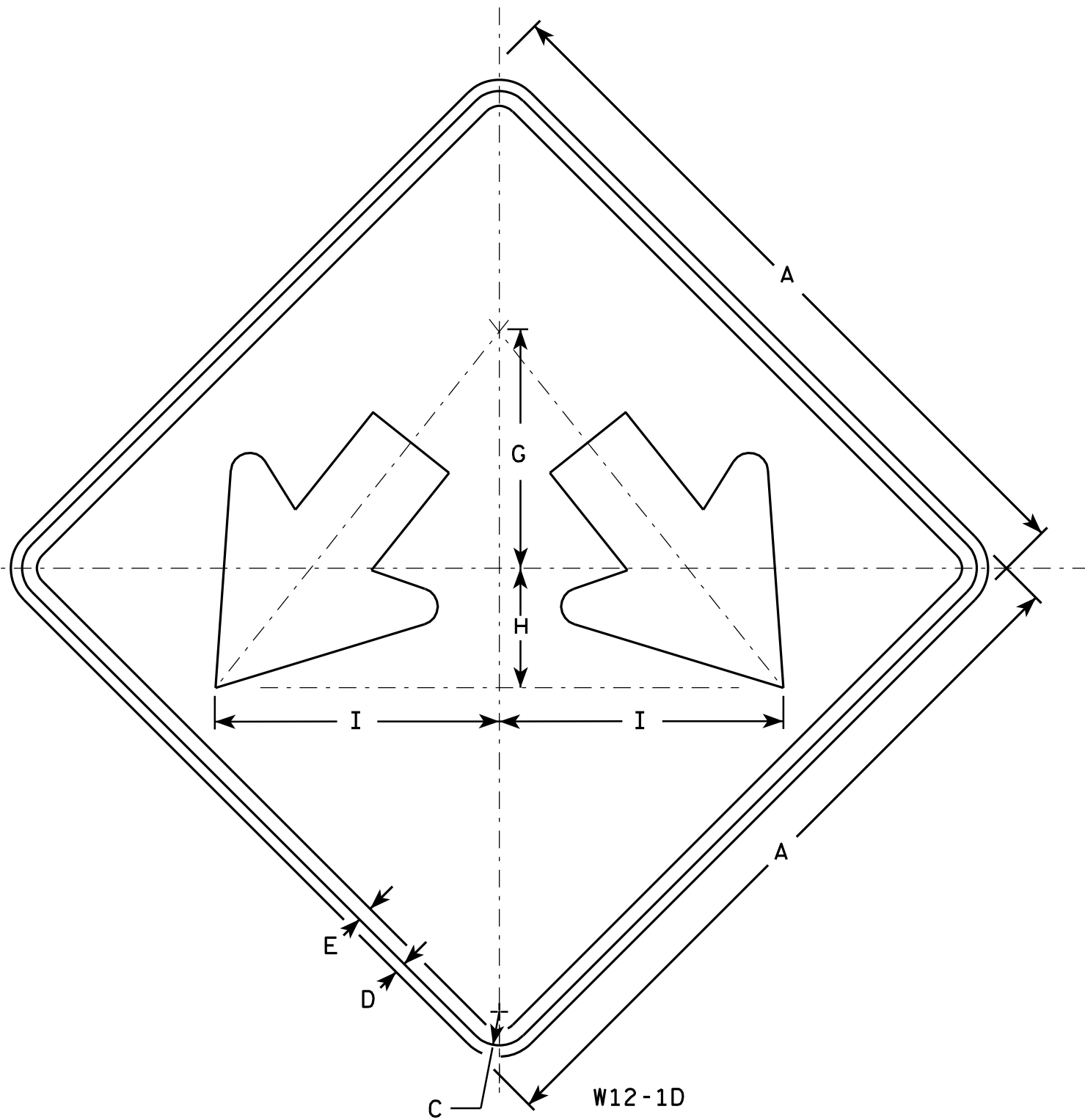
STANDARD SIGN
W11-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

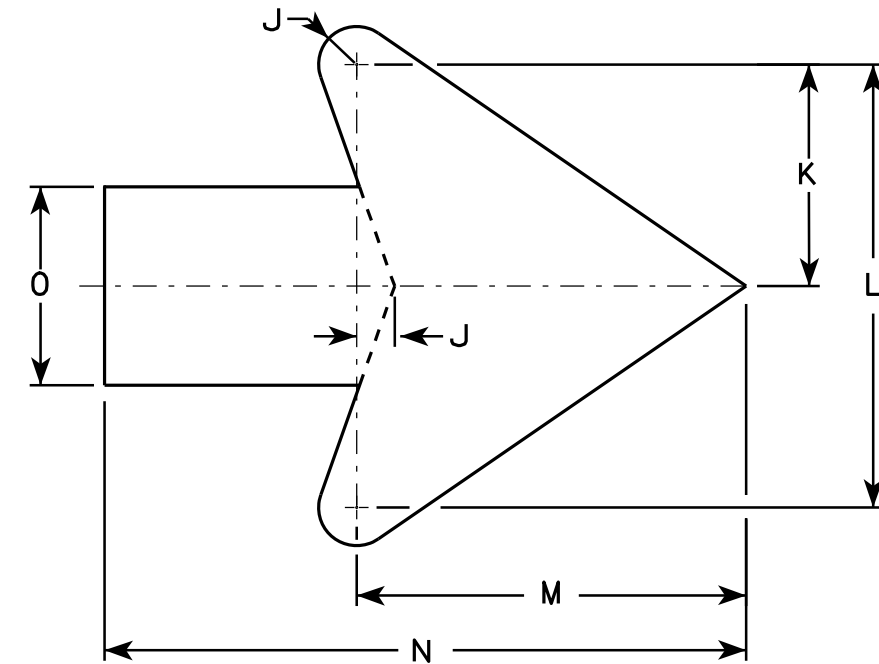
DATE 6/7/10 PLATE NO. W11-2.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
2M	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
3	30		1 3/8	1/2	5/8		10	5	11 7/8	3/4	4 1/2	9	7 7/8	13	4												6.25
4	36		1 3/8	1/2	5/8		12	6	14 1/4	1	5 1/2	10 7/8	9 5/8	15 3/4	4 3/4												9.0
5	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0

STANDARD SIGN
W12-1D

WISCONSIN DEPT OF TRANSPORTATION

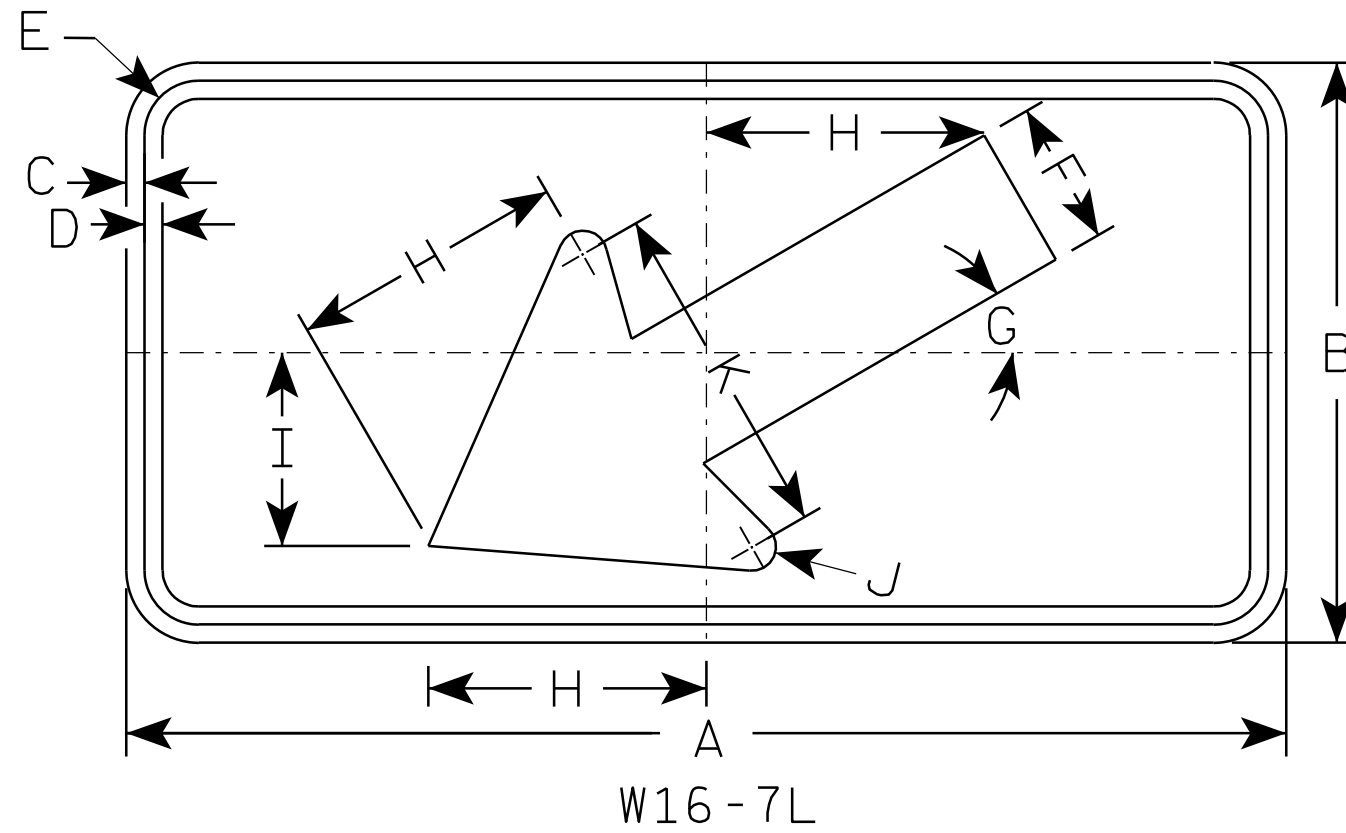
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/13/13 PLATE NO. W12-1D.15

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W16-7R is the same as W16-L except the arrow is reversed along the vertical centerline.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	3/8	3/8	1 1/8	3	30°	5 3/4	4	1/2	7																2.0
2M	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
3	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
4																											8
5																											8

STANDARD SIGN
W16-7

WISCONSIN DEPT OF TRANSPORTATION

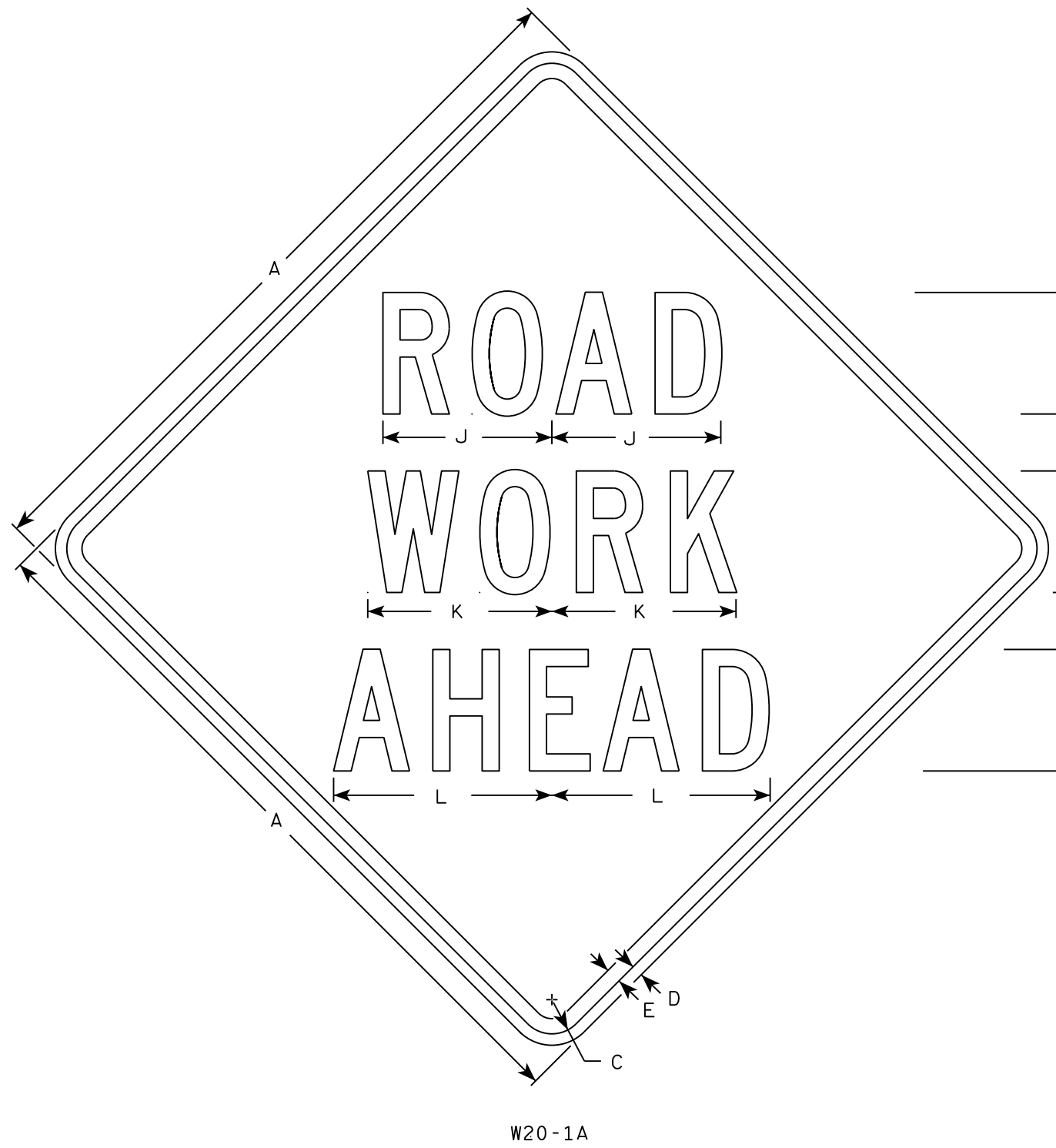
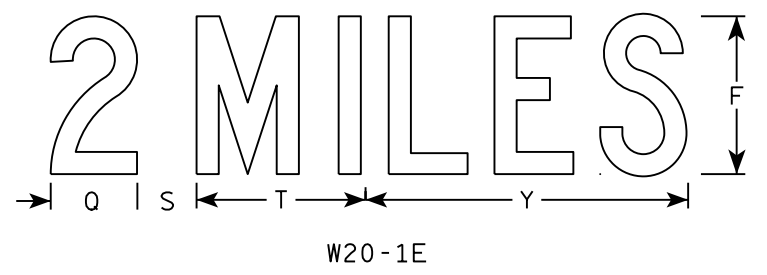
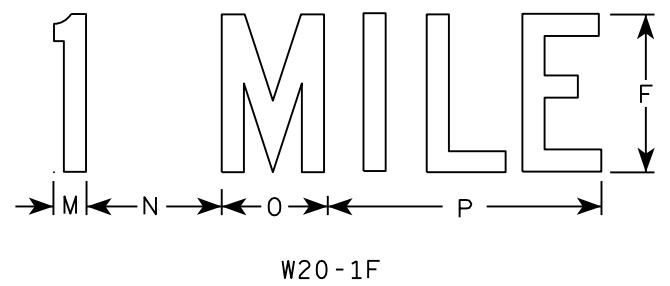
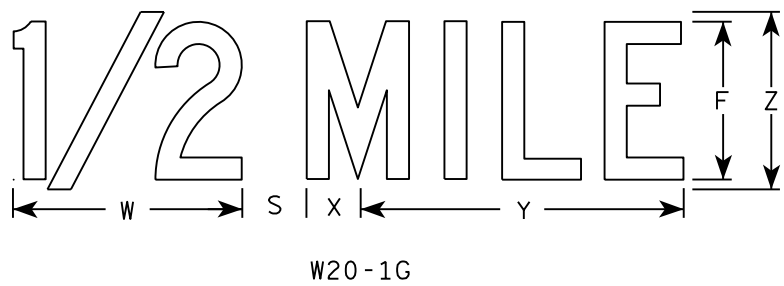
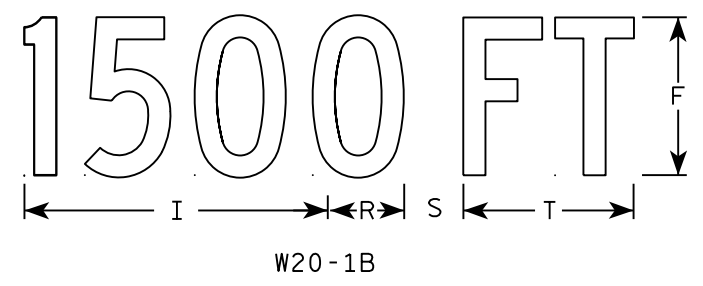
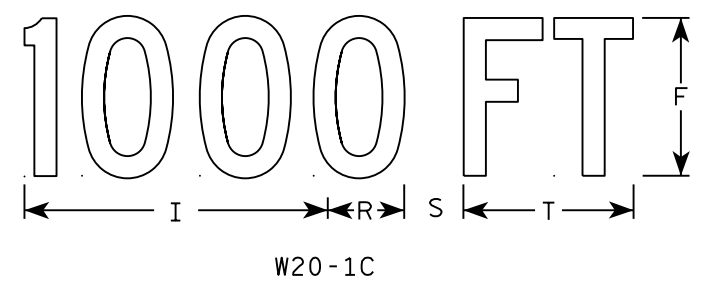
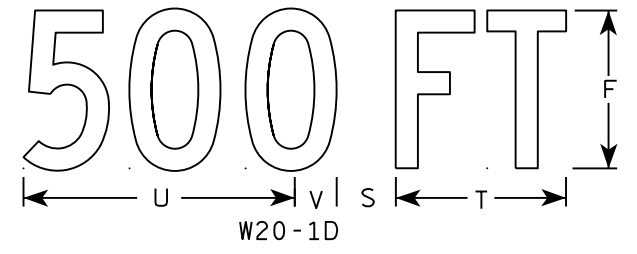
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/02/10 PLATE NO. W16-7.5

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

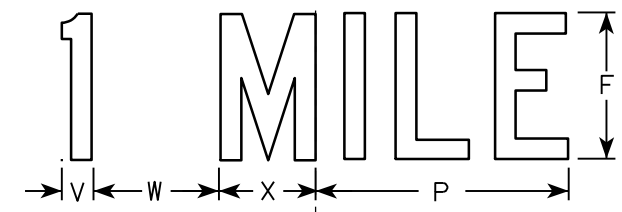
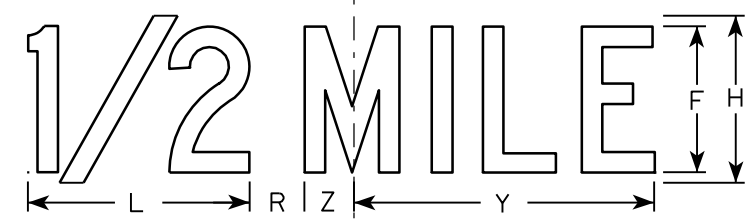
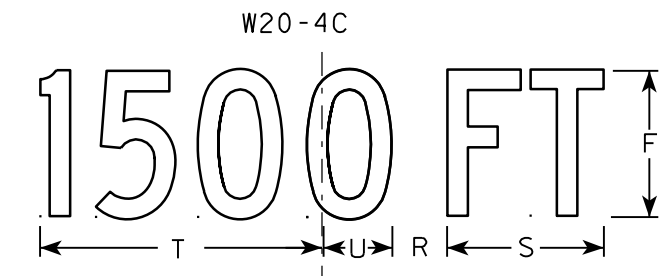
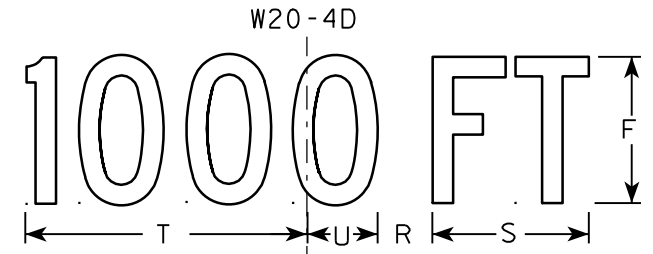
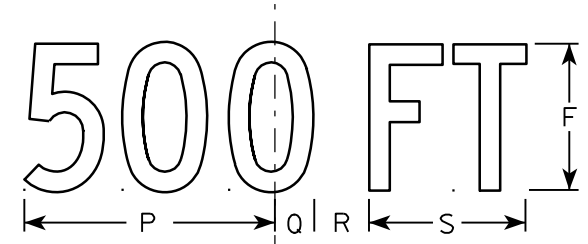
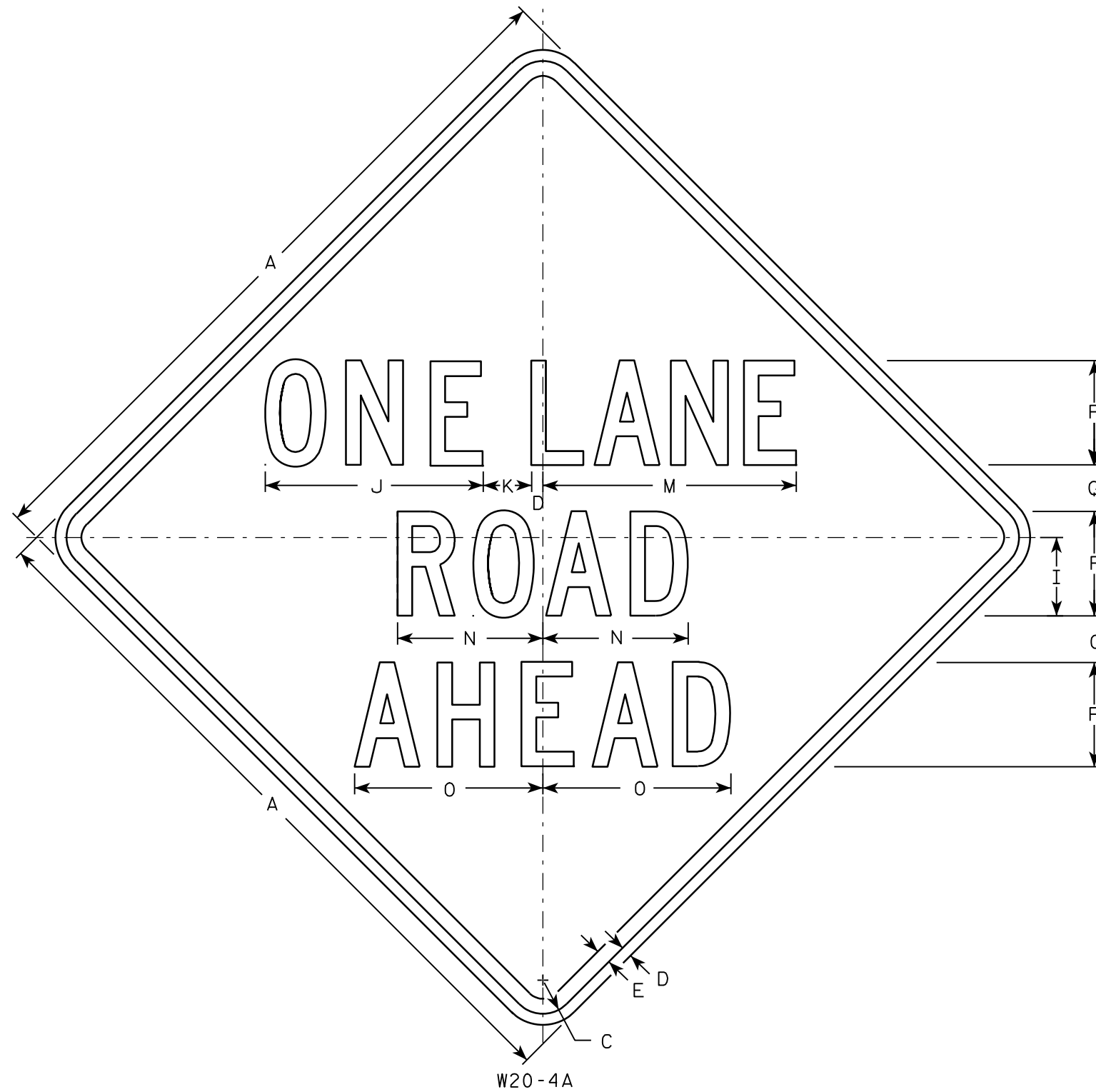


W20-1A

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, F & G
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 5/07/15 PLATE NO. W20-1.10



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

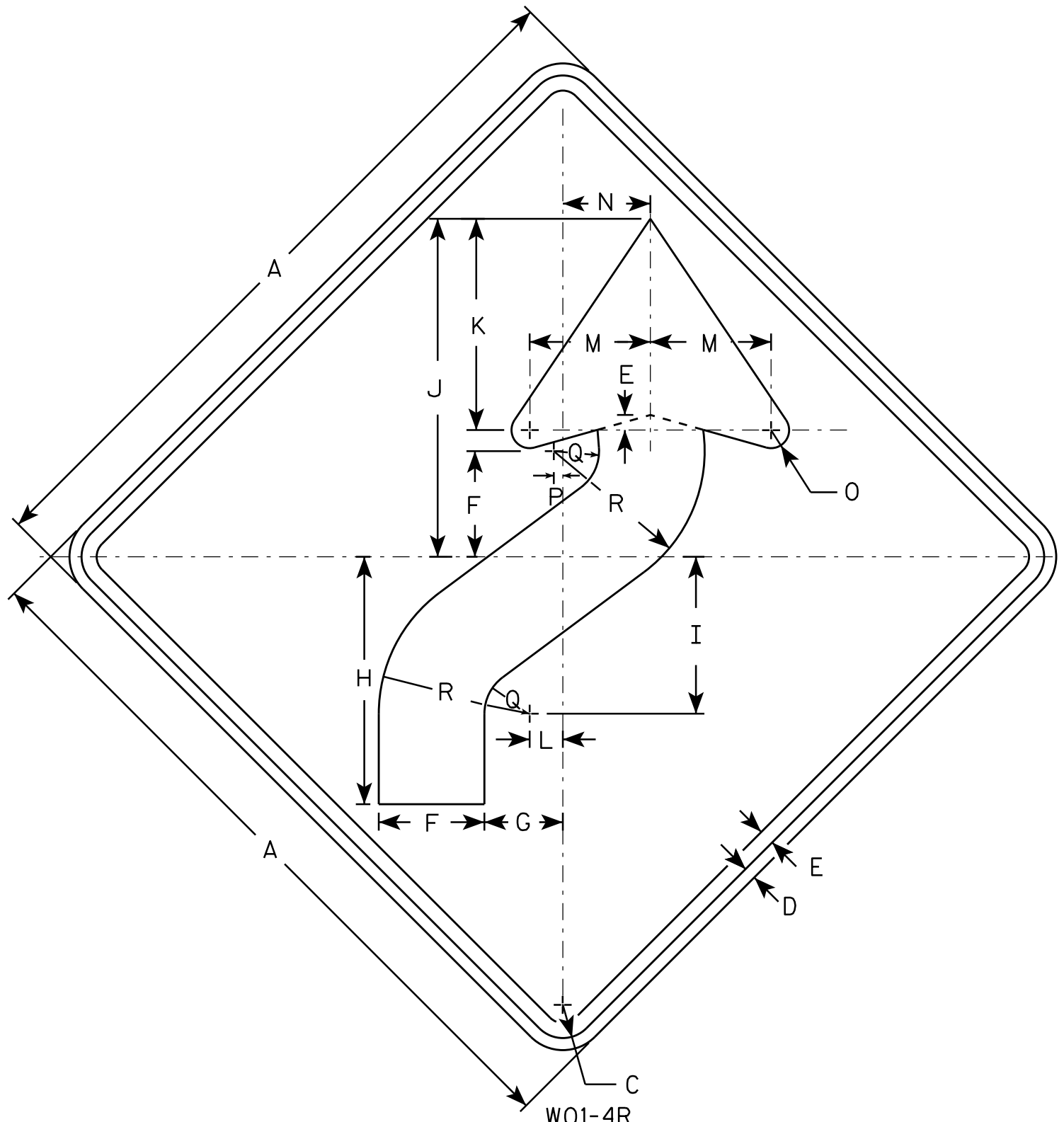
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 3/8	6	3 3/4	10 3/8	2 3/8	8	13 1/2	7	8 7/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-4A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-4.9



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

7

7

W01-4R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN
W01-4

WISCONSIN DEPT OF TRANSPORTATION

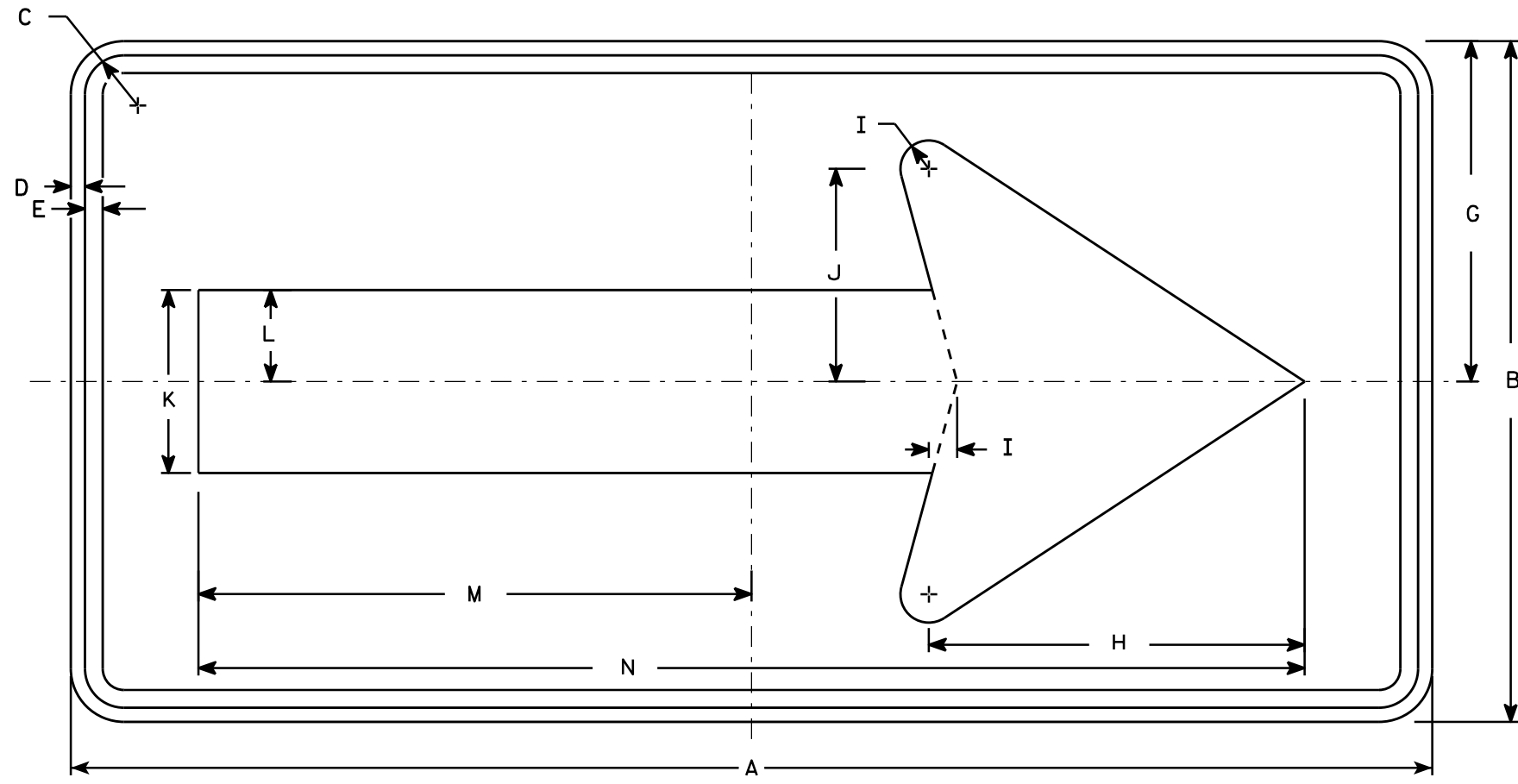
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-4.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W01-6

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

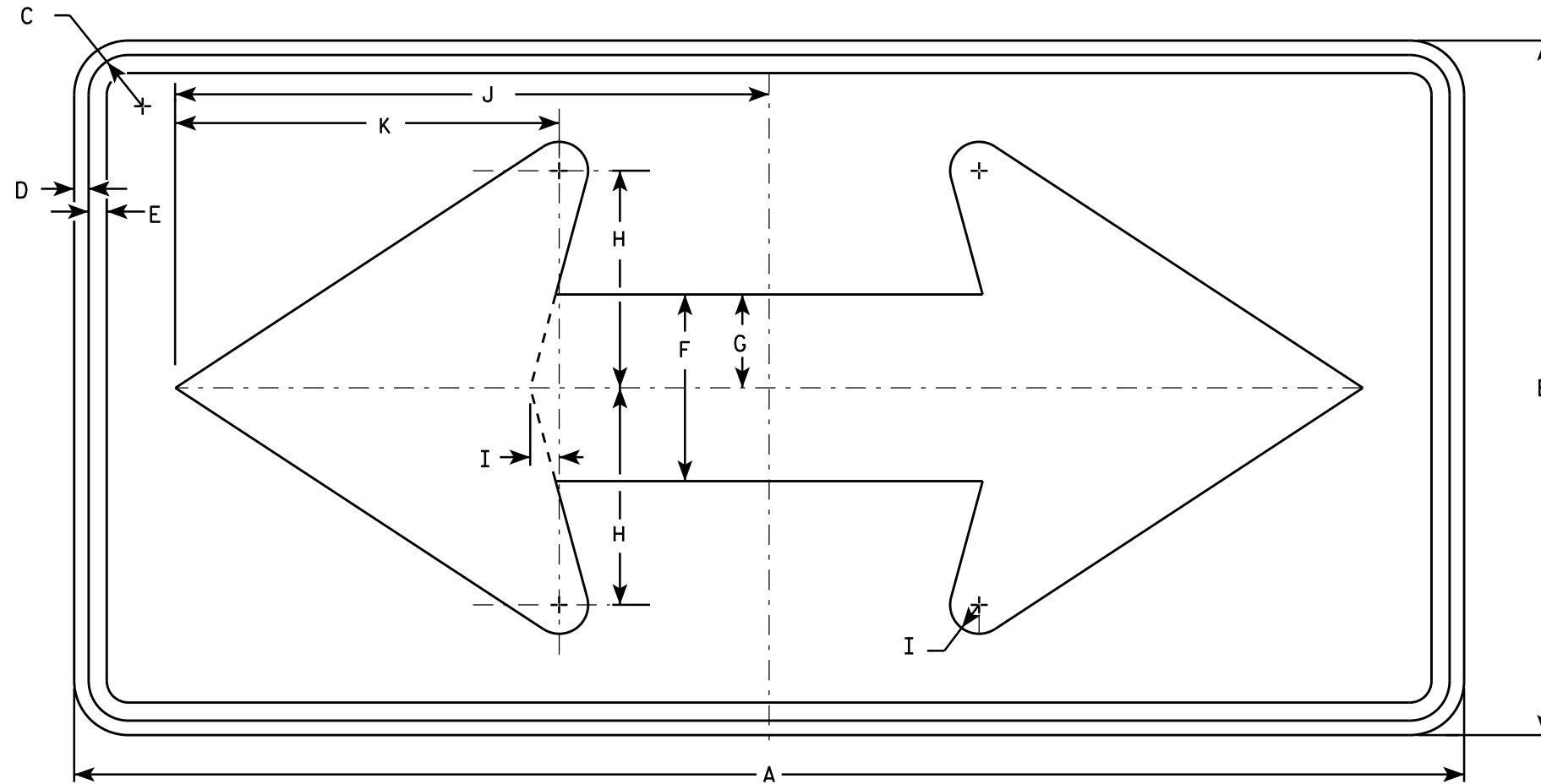
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-6.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W01-7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5

STANDARD SIGN
W01-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-7.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

DESIGN DATA

LIVE LOAD:
 DESIGN LOADING: H20
 INVENTORY RATING: HS-19
 OPERATING RATING: HS-32
 MAXIMUM STANDARD PERMIT VEHICLE LOAD: 240 KIPS

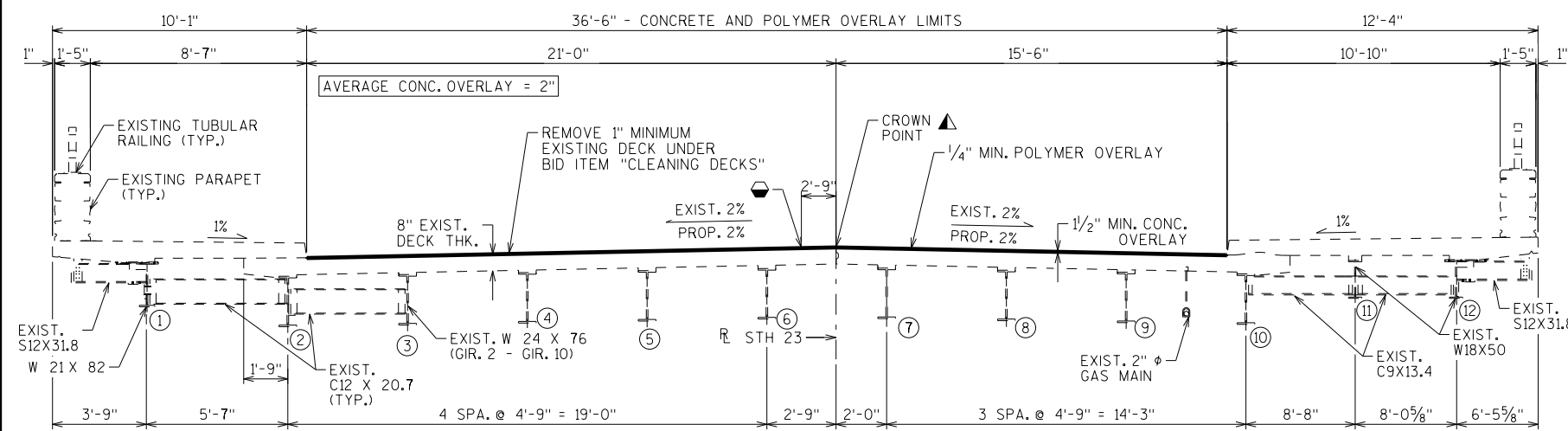
MATERIAL PROPERTIES:
 CONCRETE MASONRY OVERLAY DECKS ——— f'c = 4,000 P.S.I.

TRAFFIC VOLUME

STH 23
 ADT = 5,600 (2017)
 R.D.S. = 25 M.P.H.

GENERAL NOTES

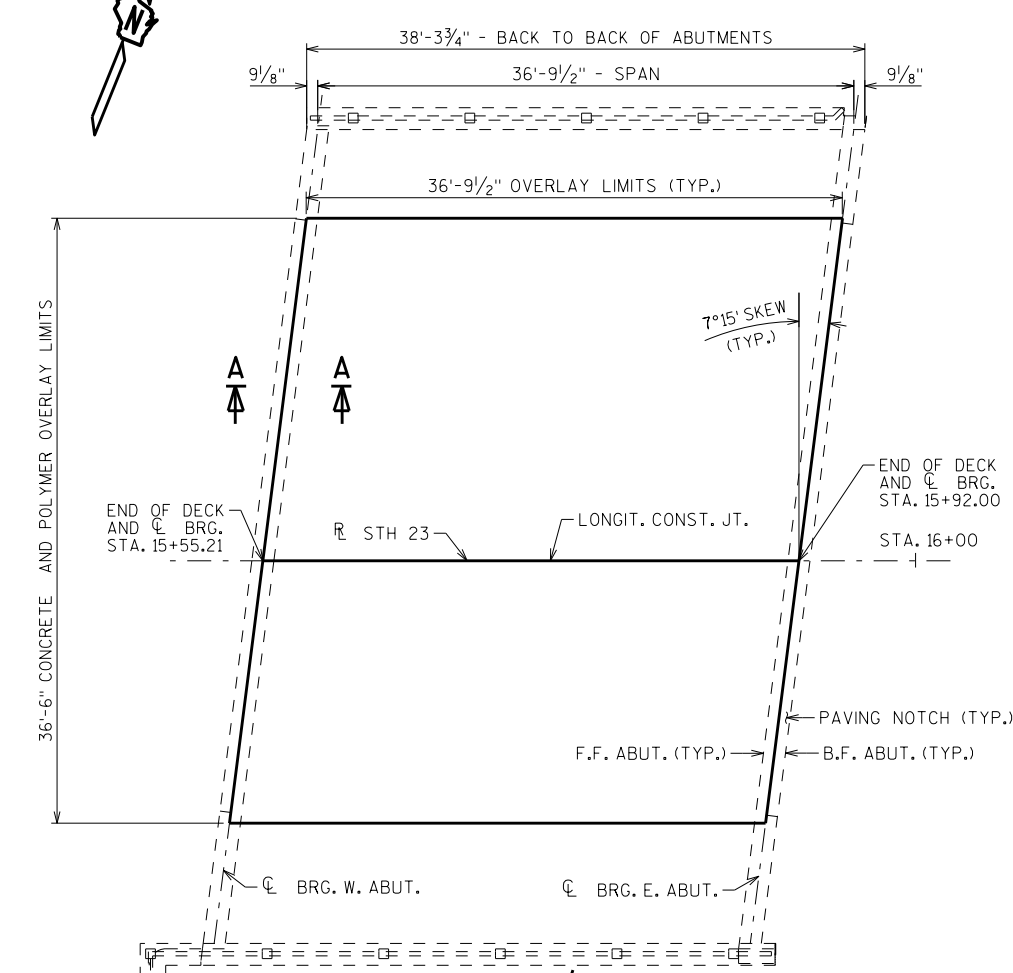
DRAWINGS SHALL NOT BE SCALED.
 DIMENSIONS SHOWN ARE BASED ON THE 1997 DECK REPLACEMENT PLANS.
 A MINIMUM OF 1-INCH OF CONCRETE SHALL BE REMOVED FROM THE ENTIRE BRIDGE DECK UNDER THE BID ITEM "CLEANING DECKS".
 "PREPARATION DECKS TYPE 1", "PREPARATION DECKS TYPE 2", AND "FULL DEPTH DECK REPAIR" ARE AS DETERMINED BY THE ENGINEER. DECK PREPARATION AREAS SHALL BE FILLED WITH "CONCRETE MASONRY OVERLAY DECKS".
 ANY EXCAVATION REQUIRED TO COMPLETE THE OVERLAY TO BE CONSIDERED INCIDENTAL TO THE BID ITEM "CONCRETE MASONRY OVERLAY DECKS".
 PROFILE GRADE LINE SHALL BE DETERMINED IN THE FIELD BASED ON A MINIMUM OVERLAY THICKNESS OF 1/2" PLACED ABOVE THE DECK SURFACE AFTER SURFACE PREPARATION. EXPECTED AVERAGE OVERLAY IS 2". IF THE AVERAGE OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.
 ALLOW CONCRETE OVERLAY TO CURE FOR A MINIMUM OF 28 DAYS BEFORE PLACING POLYMER OVERLAY.
 CONCRETE SURFACE REPAIR LOCATIONS AND EXTENTS ON BOTH ABUTMENTS SHALL BE DETERMINED BY THE FIELD ENGINEER.



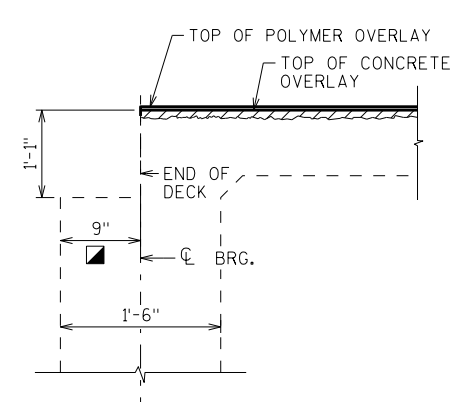
CROSS SECTION THRU ROADWAY

LOOKING UPSTATION

◐ = CONCRETE OVERLAY LONGIT. CONST. JOINT
 ▲ = POLYMER OVERLAY LONGIT. CONST. JOINT



PLAN



SECTION A-A

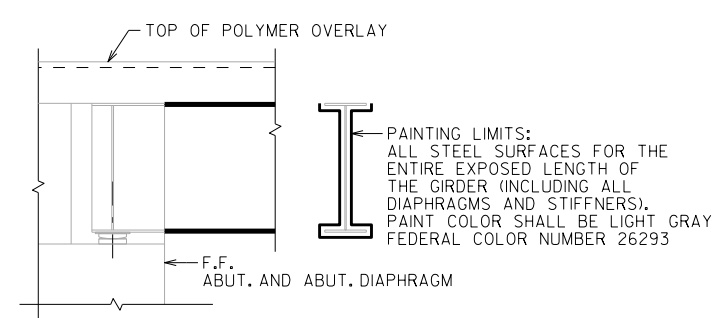
■ MEASURED NORMAL TO ABUTMENT

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.0301	PREPARATION DECKS TYPE 1	SY	15
509.0302	PREPARATION DECKS TYPE 2	SY	5
509.0500	CLEANING DECKS	SY	150
509.1500	CONCRETE SURFACE REPAIR	SF	5
509.2000	FULL-DEPTH DECK REPAIR	SY	1
509.2500	CONCRETE MASONRY OVERLAY DECKS	CY	9
509.5100.S	POLYMER OVERLAY	SY	150
517.1800.S	STRUCTURE REPAINTING RECYCLED ABRASIVE B-39-970	LS	1
517.4500.S	NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS B-39-970	LS	1
517.6001.S	PORTABLE DECONTAMINATION FACILITY	EACH	1

LIST OF DRAWINGS

1. CONCRETE & POLYMER OVERLAY
2. CONCRETE OVERLAY STAGING
3. POLYMER OVERLAY STAGING



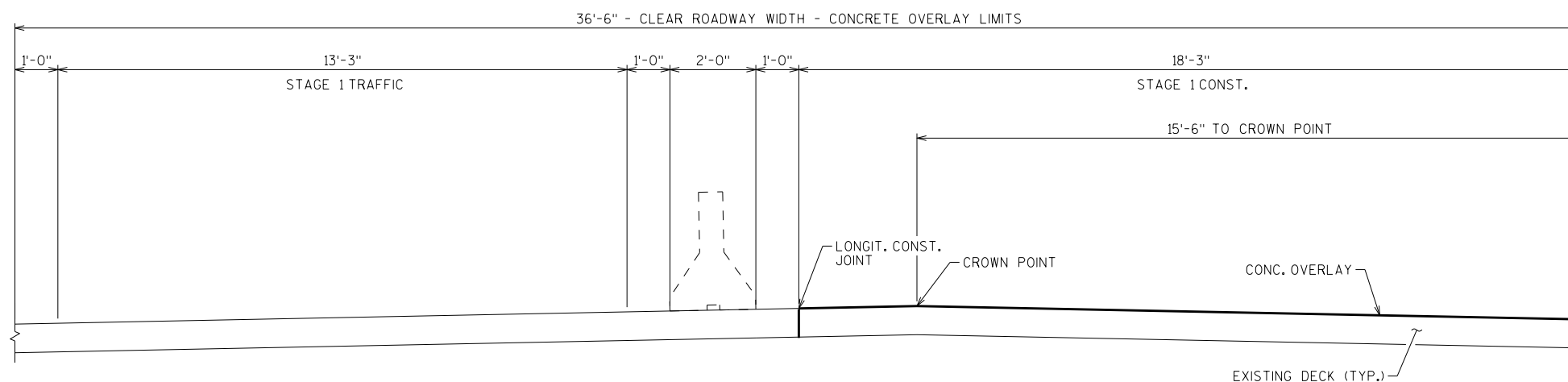
PAINTING DETAIL

(TYP. AT BOTH ABUTMENTS)

STRUCTURE DESIGN CONTACTS:

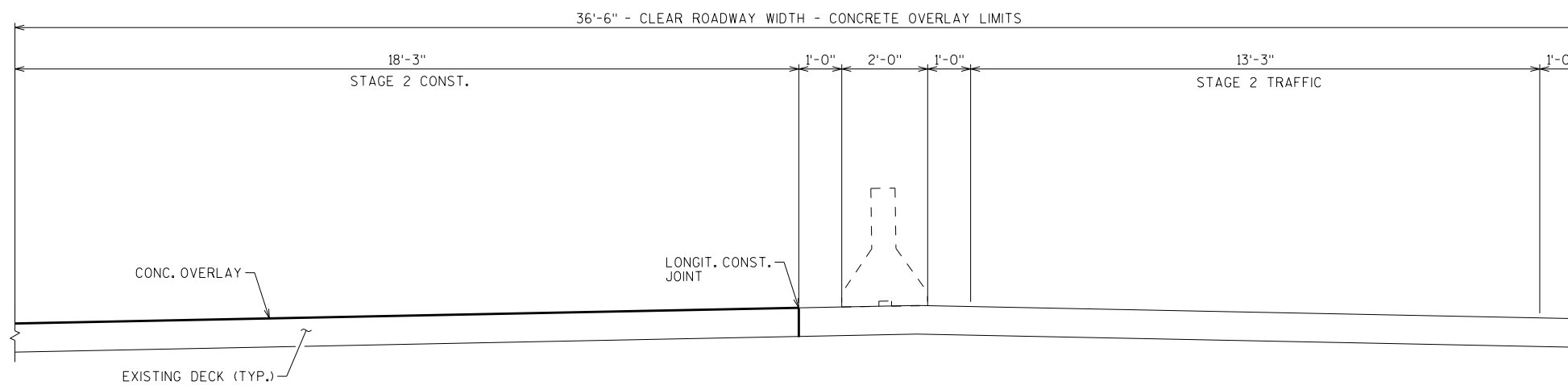
JOEL MAAS (608) 267-0273
 LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY
 BUREAU OF STRUCTURES ACCEPTED <i>William C. Decker</i> 2/16/18 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-39-970			
STH 23 OVER MONTELLO RIVER			
COUNTY	MARQUETTE	CITY	MONTELLO
DESIGN SPEC.	REHABILITATION N/A		
DESIGNED BY	JDM	DESIGNED CK'D.	DLM
DRAWN BY	JDM	PLANS CK'D.	DLM
CONCRETE & POLYMER OVERLAY			SHEET 1 OF 3



CONCRETE OVERLAY STAGE 1 CONSTRUCTION

■ (LOOKING UPSTATION)



CONCRETE OVERLAY STAGE 2 CONSTRUCTION

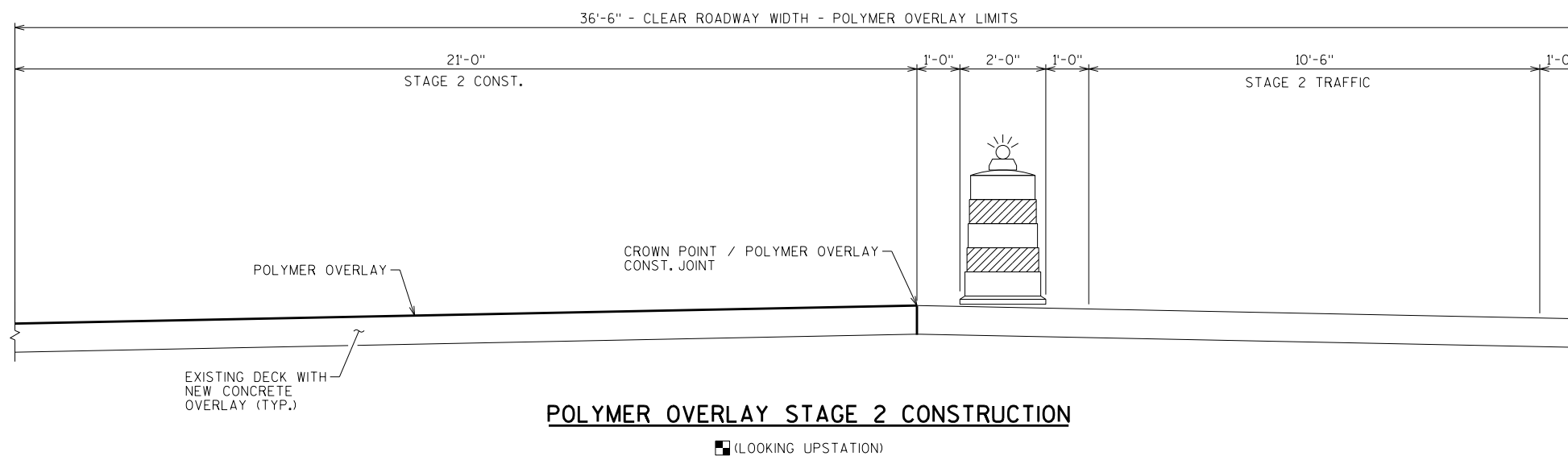
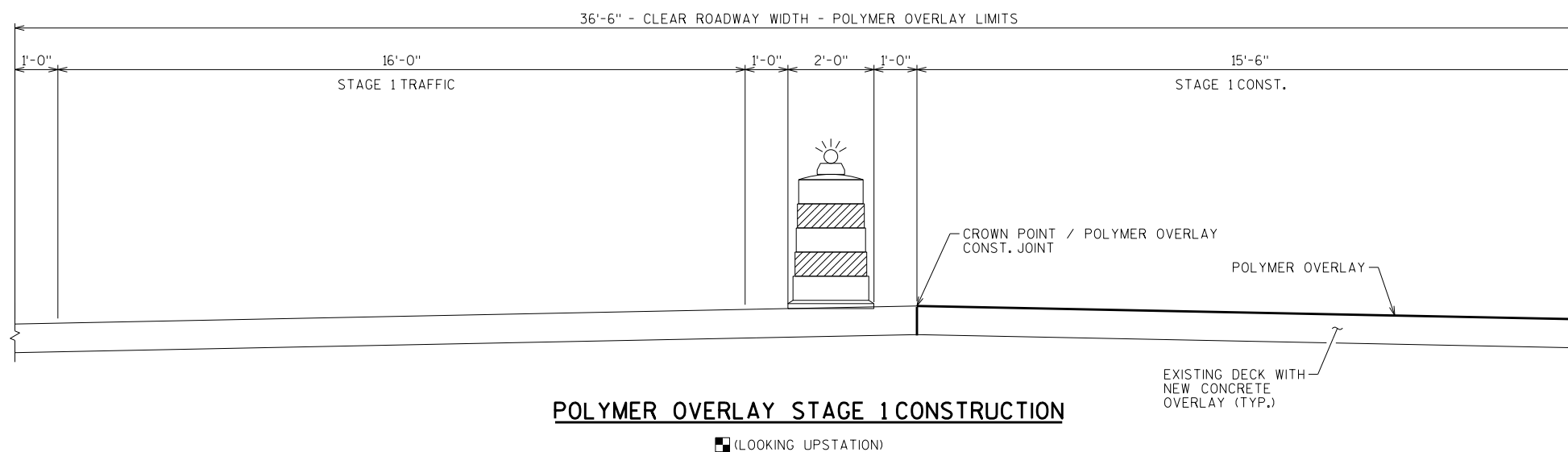
■ (LOOKING UPSTATION)

8

8

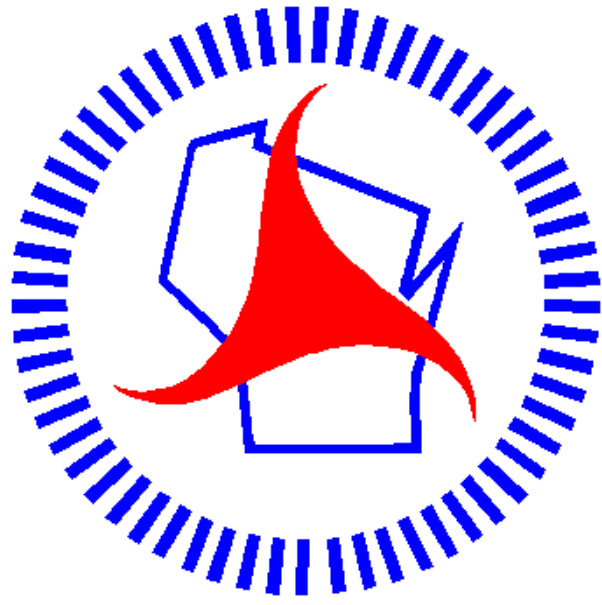
■ NOTE: OTHER SUPERSTRUCTURE COMPONENTS NOT SHOWN FOR CLARITY. SEE SHEET 1 FOR FULL CROSS-SECTION DETAILS

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-39-970			
DRAWN BY JDM		PLANS CK'D. DLM	
CONCRETE OVERLAY STAGING			SHEET 2



NOTE: OTHER SUPERSTRUCTURE COMPONENTS NOT SHOWN FOR CLARITY. SEE SHEET 1 FOR FULL CROSS-SECTION DETAILS

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-39-970			
DRAWN BY JDM		PLANS CK'D. DLM	
POLYMER OVERLAY STAGING			SHEET 3



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

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WIS MAY 2018

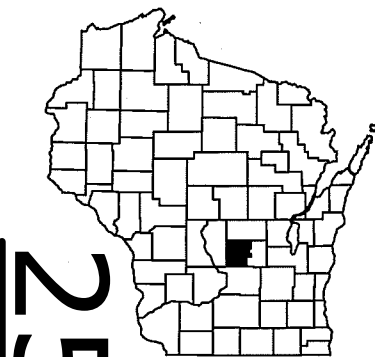
PROJECT ID: 6150-00-61
WITH: 6054-05-75, 6180-00-72

COUNTY: MARQUETTE

ORDER OF SHEETS

- Section No. 1 Title
- Section No. 2 Typical Sections and Details
- Section No. 3 Estimate of Quantities
- Section No. 3 Miscellaneous Quantities
- ~~Section No. 4 Right of Way Plat~~
- Section No. 5 Plan and Profile
- Section No. 6 Standard Detail Drawings
- ~~Section No. 7 Sign Plates~~
- Section No. 8 Structure Plans
- ~~Section No. 9 Computer Earthwork Data~~
- ~~Section No. 9 Cross Sections~~

TOTAL SHEETS = 18



DESIGN DESIGNATION 6150-00-31

A.A.D.T. 2017 = 8600
 A.A.D.T. 2027 = 9700
 D.H.V. = 8.3
 D.D. =
 T. =
 DESIGN SPEED =
 ESALS =

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	
EXISTING RIGHT OF WAY		GRADE ELEVATION	
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE		ELECTRIC	
EXISTING CULVERT		FIBER OPTIC	
PROPOSED CULVERT (Box or Pipe)		GAS	
COMBUSTIBLE FLUIDS		SANITARY SEWER	
MARSH AREA		STORM SEWER	
WOODED OR SHRUB AREA		TELEPHONE	
		WATER	
		UTILITY PEDESTAL	
		POWER POLE	
		TELEPHONE POLE	

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

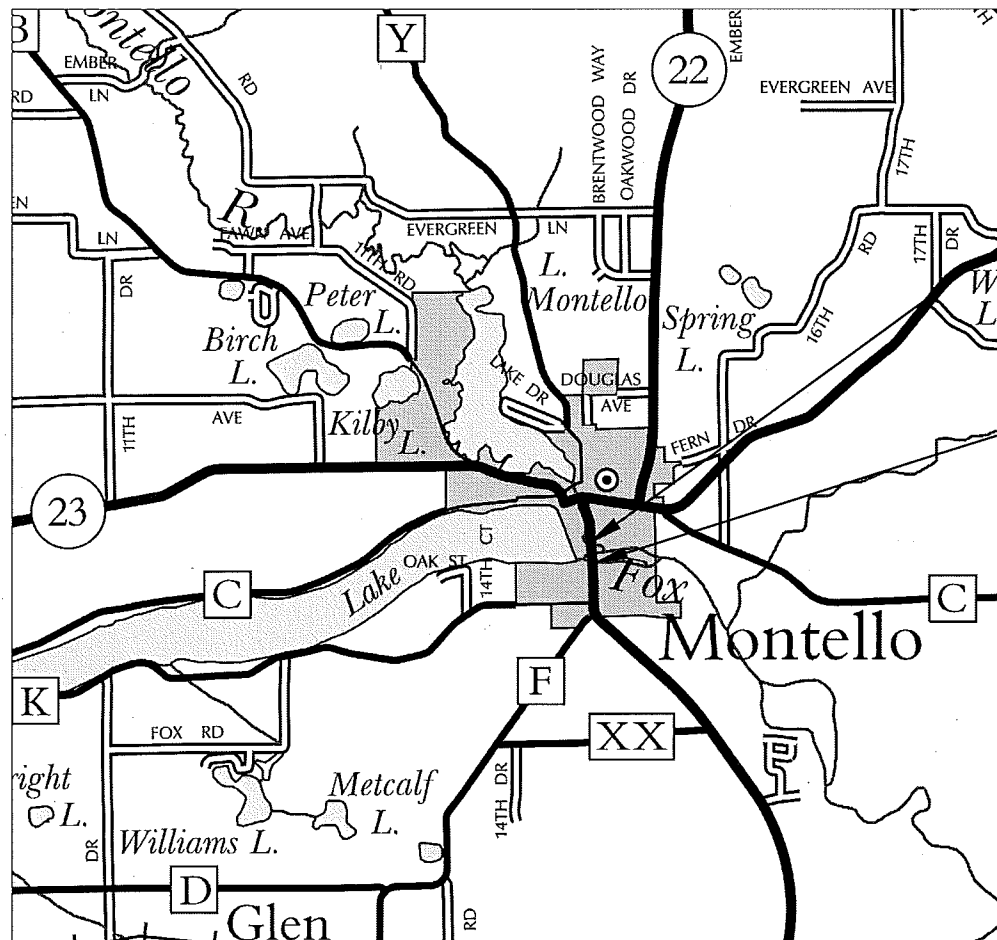
PLAN OF PROPOSED IMPROVEMENT

C MONTELLO, MAIN STREET

FOX & MONTELLO RIVER BR B-39-49, 51

STH 22
MARQUETTE

STATE PROJECT NUMBER
6150-00-61



LAYOUT
 SCALE 0 1 MILE

TOTAL NET LENGTH OF CENTERLINE = 1.40

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, MARQUETTE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6150-00-61		

END PROJECT 6150-00-61
 STA 651+33
 STRUCTURE B-39-51

BEGIN PROJECT 6150-00-61
 STA 643+12
 STRUCTURE B-39-49

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

PREPARED BY
 Surveyor: NC REGION
 Designer: REBECCA OLSEN
 Project Manager: DAN HOLLOWAY
 Regional Examiner: CHERYL SIMON
 Regional Supervisor: NICHOLE LYSNE

APPROVED FOR THE DEPARTMENT
 DATE: 3/9/18 (Signature)

E

UTILITIES**ADAMS-COLUMBIA ELECTRIC COOPERATIVE -
ELECTRICITY**

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FRIENDSHIP WI 53934
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E-MAIL: dmoore@acecwi.com

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ALLIANT ENERGY - ELECTRICITY

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RIPON WI 54971
PHONE: (920)748-4011
MOBILE: (920)238-1137
E-MAIL: matthewschmitz@alliantenergy.com

CHARTER COMMUNICATIONS - COMMUNICATION LINE

NICK FRASE
N3760 COUNTY ROAD DJ
JUNEAU WI 53039
PHONE: (920) 304-6797
EMAIL: nick.frase@charter.com

**FRONTIER COMMUNICATIONS OF WI LLC -
COMMUNICATION LINE**

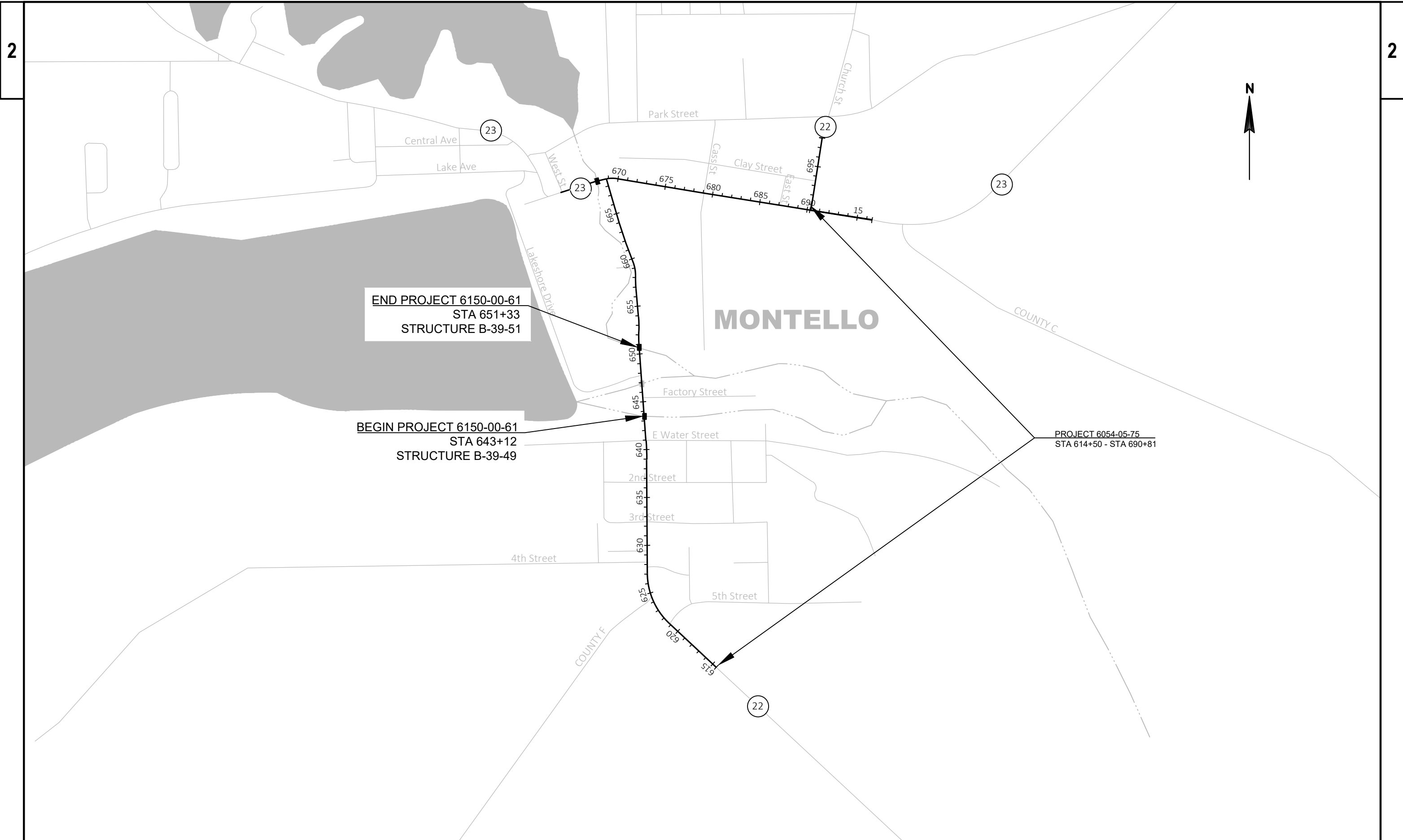
JERRY MOORE
2222 WEST WISCONSIN STREET
PORTAGE WI 53901
PHONE: (608) 742-9507
MOBILE: (608)346-0353
E-MAIL: jerald.r.moore@fr.com

DNR

DNR NORTHEAST REGIONAL HEADQUARTERS
2984 SHAWANO AVE
GREEN BAY, WI 54313
PHONE: (920)662-5130

DIGGERS  HOTLINE

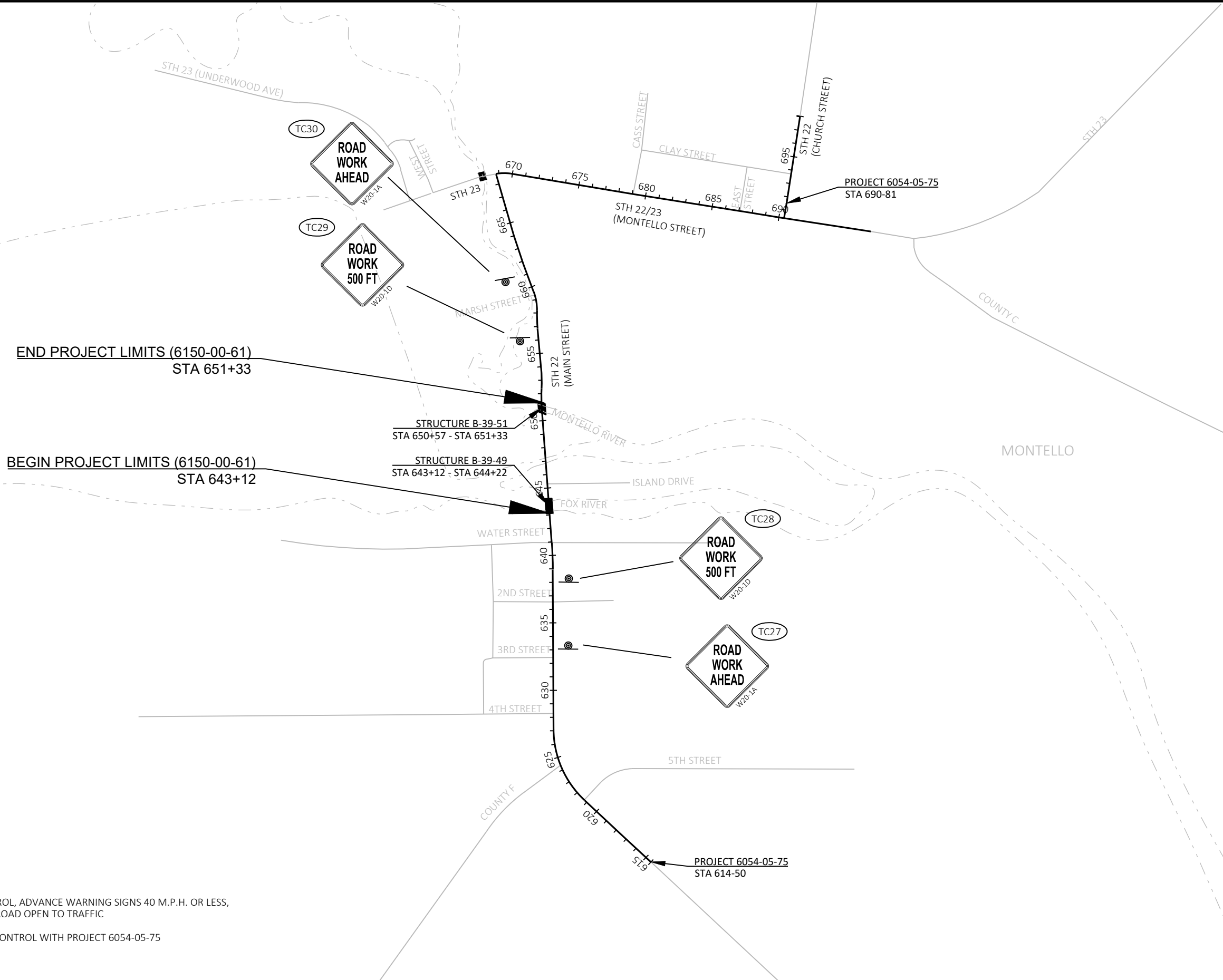
Dial  or (800)242-8511
www.DiggersHotline.com



2

2

PROJECT NO: 6150-00-61	HWY: STH 22	COUNTY: MARQUETTE	PROJECT OVERVIEW	SHEET	E
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SEE SDD TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS,
TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC

COORDINATE TRAFFIC CONTROL WITH PROJECT 6054-05-75

PROJECT NO: 6150-00-61	HWY: STH 22	COUNTY: MARQUETTE	TRAFFIC CONTROL	SHEET	E
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Estimate Of Quantities By Plan Sets

6150-00-61

Line	Item	Item Description	Unit	Total	Qty
0016	213.0100	Finishing Roadway (project) 02. 6150-00-61	EACH	1.000	1.000
0082	606.0100	Riprap Light	CY	63.000	63.000
0098	619.1000	Mobilization	EACH	0.300	0.300
0134	643.0300	Traffic Control Drums	DAY	560.000	560.000
0146	643.0900	Traffic Control Signs	DAY	320.000	320.000
0150	643.5000	Traffic Control	EACH	0.300	0.300
0218	SPV.0035	Special 01. Scour Repair Grout	CY	15.000	15.000
0220	SPV.0035	Special 02. Scour Repair Grout Bags	CY	37.000	37.000
0222	SPV.0035	Special 03. Scour Repair Grout Mats 4-Inch	CY	21.000	21.000

3

MOBILIZATION

PROJECT	619.1000 EACH	REMARKS
6150-00-61	0.15	CATEGORY 0010
6150-00-61	0.15	CATEGORY 0020
PROJECT TOTAL:		0.30

TRAFFIC CONTROL DRUMS

LOCATION	DAYS NEEDED	NUMBER REQUIRED	643.0300 DAY	REMARKS
STH 22 (B-39-49)	5	56	280	CATEGORY 0010
STH 22 (B-39-51)	5	56	280	CATEGORY 0020
PROJECT TOTAL:			560	

TRAFFIC CONTROL

LOCATION	643.5000 DAY	REMARKS
STH 22 (B-39-49)	0.15	CATEGORY 0010
STH 22 (B-39-51A)	0.15	CATEGORY 0020
PROJECT TOTAL:		0.30

3

FINISHING ROADWAY (PROJECT)

STATION - STATION	213.0100 02. (6150-00-61) EACH	REMARKS
643+12 - 651+33	0.50	CATEGORY 0010
643+12 - 651+33	0.50	CATEGORY 0020
PROJECT TOTAL:		1

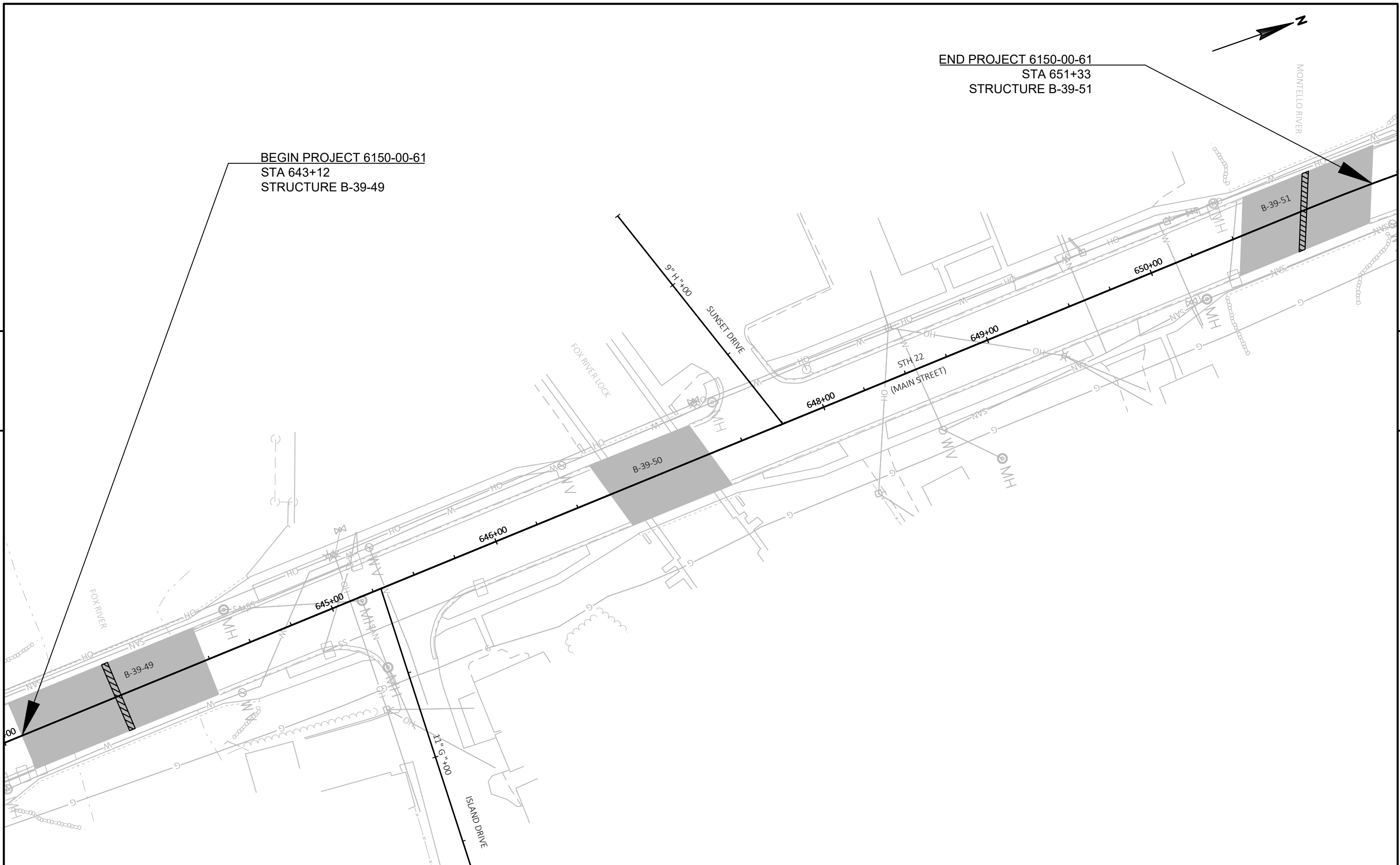
TRAFFIC CONTROL SIGNS

SIGN NO.	SIGN CODE	MESSAGE	SIZE	643.0900		CAT 0010 DAYS	CAT 0020 DAYS	REMARKS
				TYPE II REFLECTIVE H	TYPE II REFLECTIVE F			
TC27	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	40	---	CATEGORY 0010
TC28	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	40	---	CATEGORY 0010
TC29	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	40	---	CATEGORY 0010
TC30	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	40	---	CATEGORY 0010
TC27	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	---	40	CATEGORY 0020
TC28	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	---	40	CATEGORY 0020
TC29	W20-1D	ROAD WORK 500 FT	48"X48"	---	1	---	40	CATEGORY 0020
TC30	W20-1A	ROAD WORK AHEAD	48"X48"	---	1	---	40	CATEGORY 0020
CATEGORY SUBTOTALS:						160	160	
PROJECT TOTALS:						320		



END PROJECT 6150-00-61
STA 651+33
STRUCTURE B-39-51

BEGIN PROJECT 6150-00-61
STA 643+12
STRUCTURE B-39-49



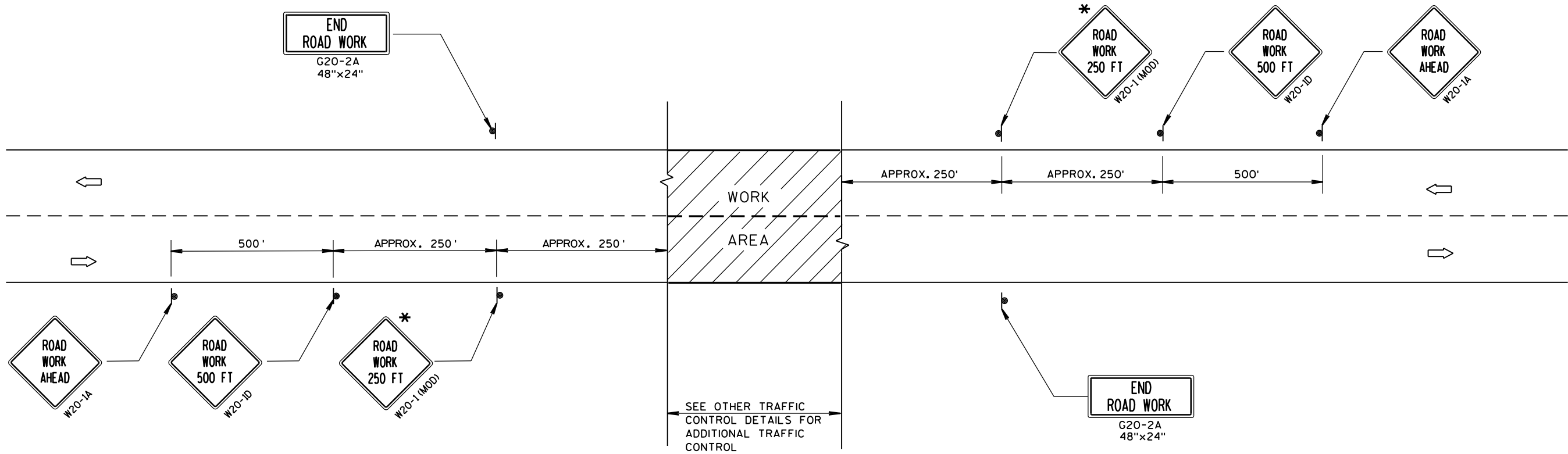
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5

PROJECT NO: 6150-00-61	HWY: STH 22	COUNTY: MARQUETTE	PLAN	SHEET	E
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Standard Detail Drawing List

15C05-04	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C11-07B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

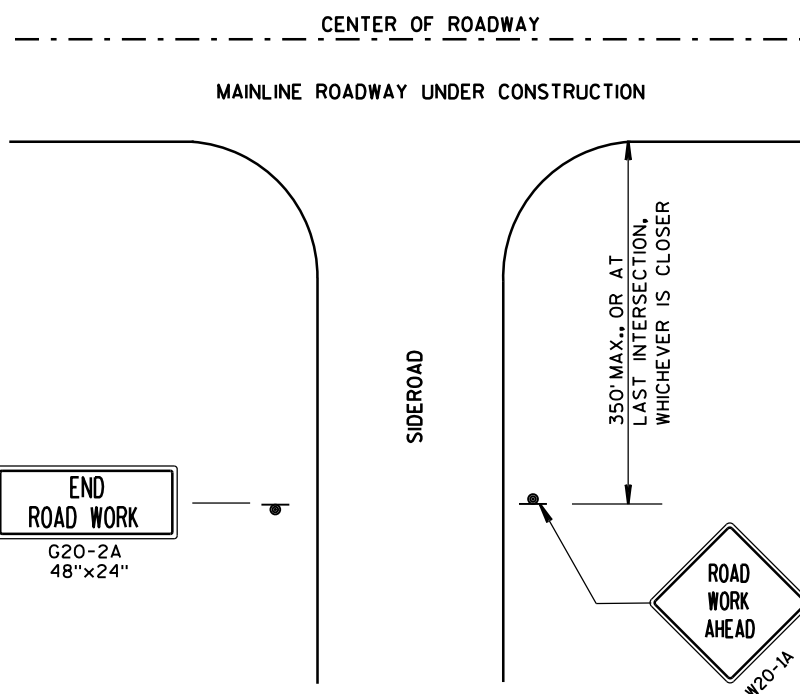
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

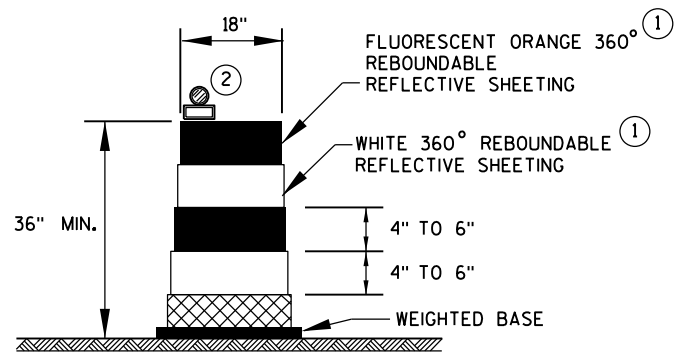
IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

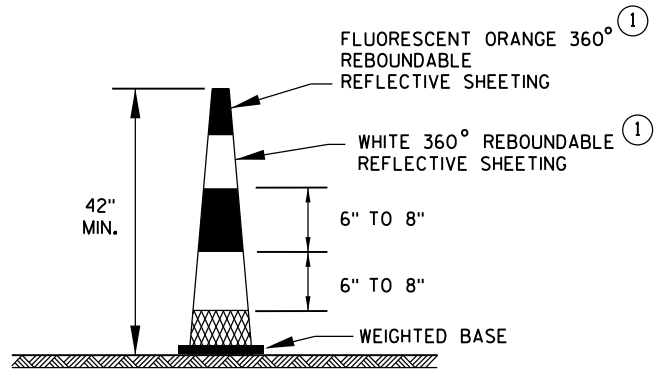


- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - DIRECTION OF TRAFFIC
 - WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



DRUM

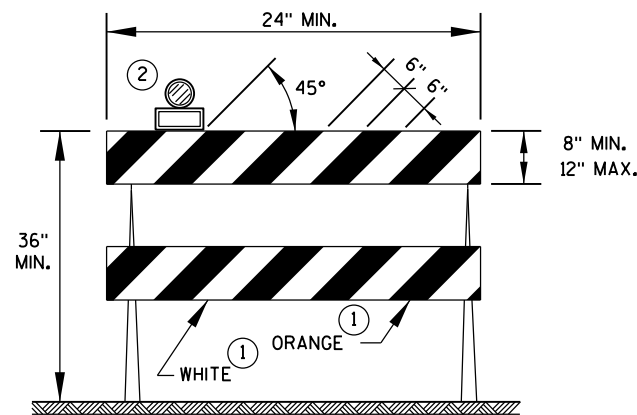


42" CONE

DO NOT USE IN TAPERS
 1/2 SPACING OF DRUMS

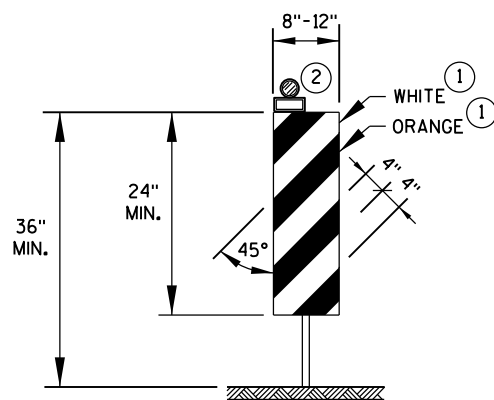
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



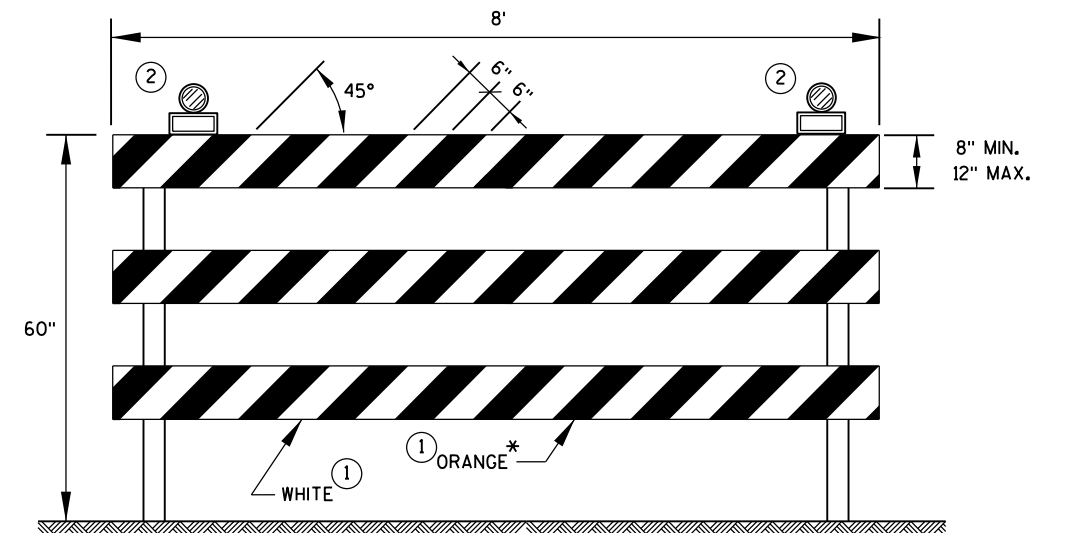
TYPE 2 BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED.
 ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE 3 BARRICADE

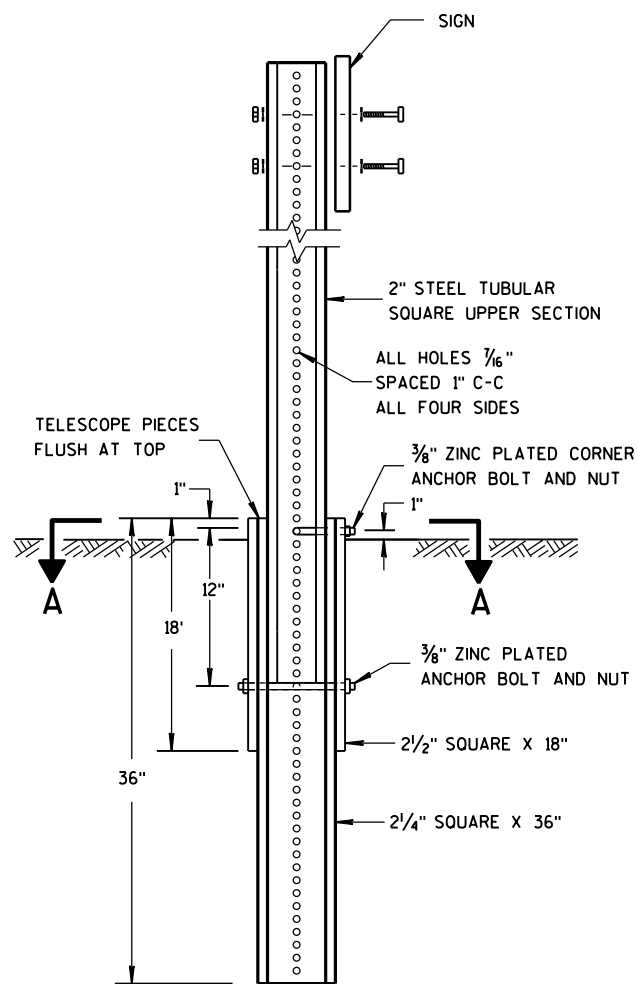
IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION, USE RED SHEETING.

6

6

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



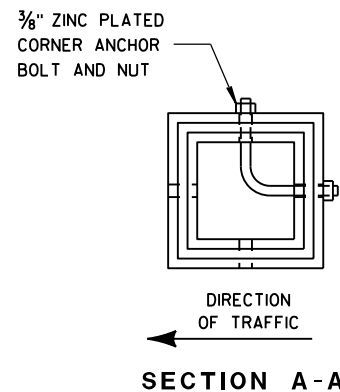
DETAIL OF TUBULAR STEEL SIGN POST

TUBULAR STEEL POSTS

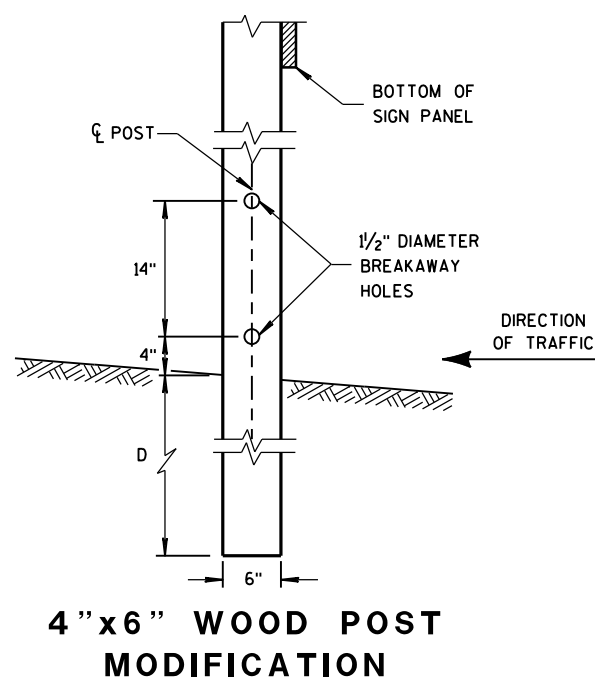
AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

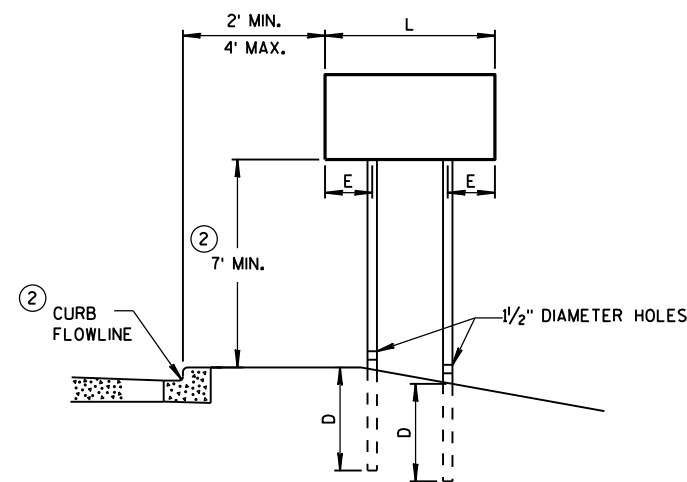
SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.



SECTION A-A



4" X 6" WOOD POST MODIFICATION

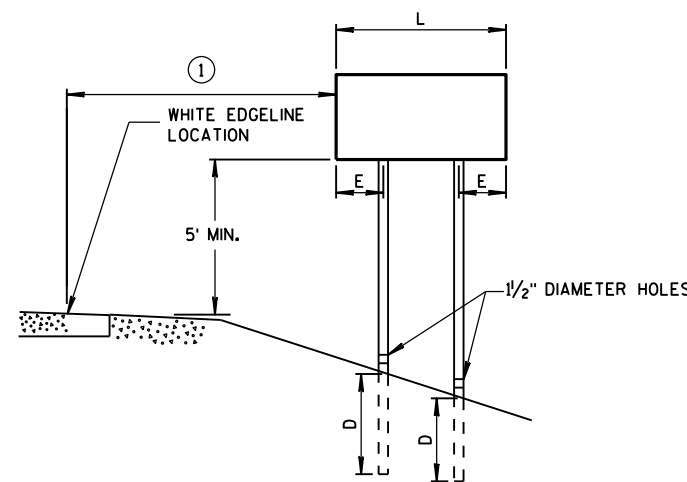


URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



RURAL AREA

4" X 6" WOOD POST

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48" OR LESS AND LESS THAN 20 SQ. FT.	-	1
LESS THAN 60"	12"	2
60" TO 120"	L/5	2
GREATER THAN 120" LESS THAN 168"	12"	3
168" AND GREATER	12"	4

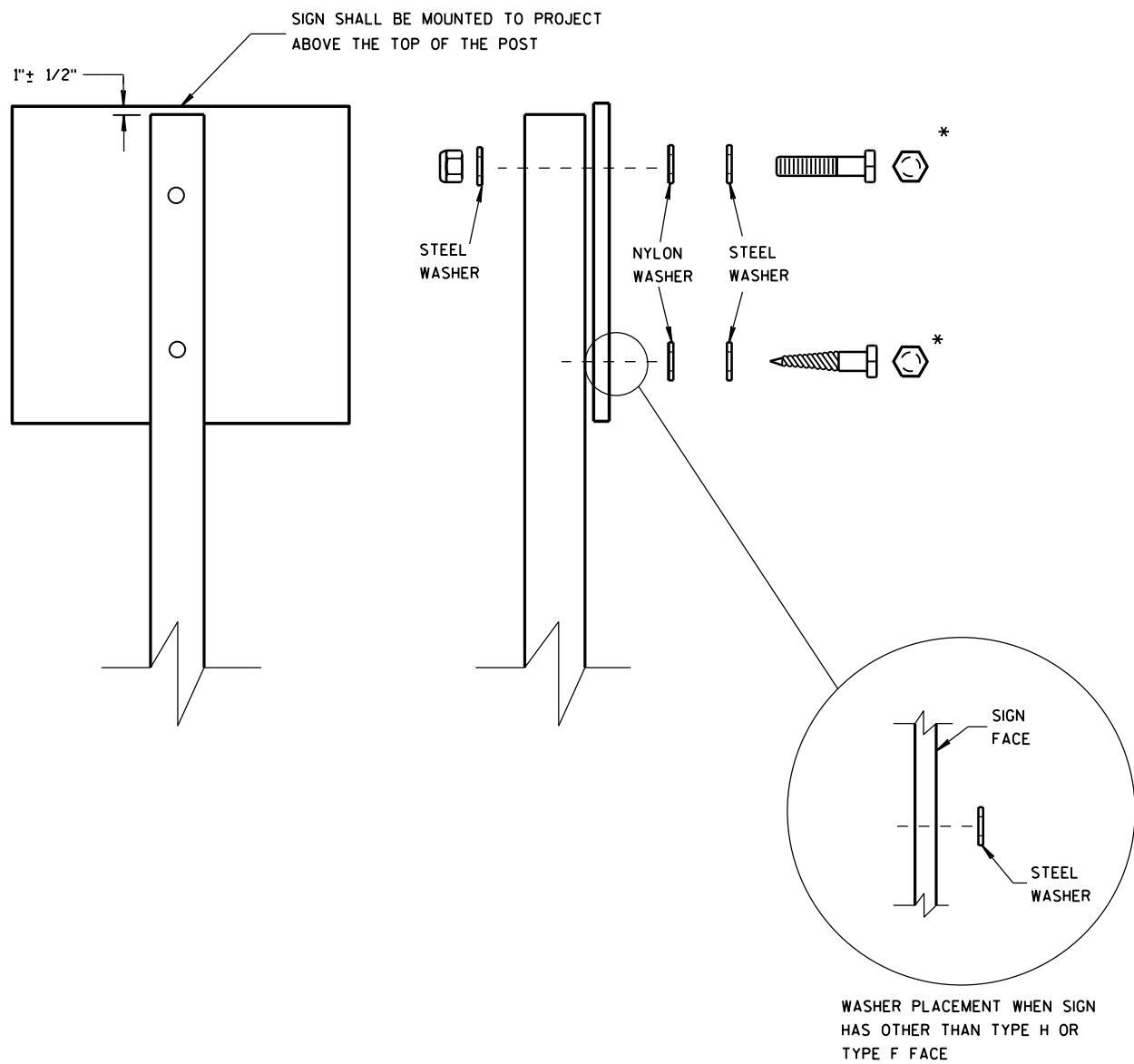
SEE NOTE ③

GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3

B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3"

MACHINE BOLTS - 5/16" X 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS

RIVETS - 5/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

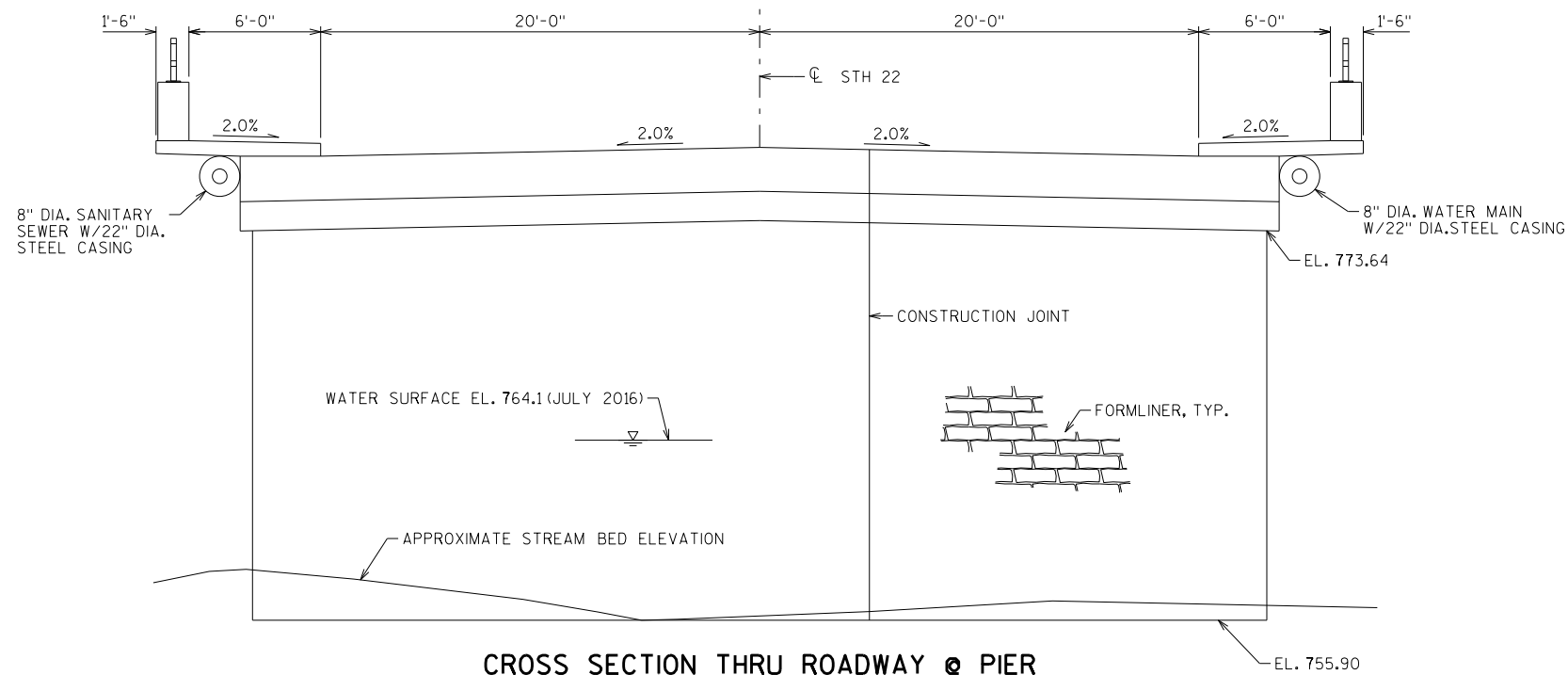
WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

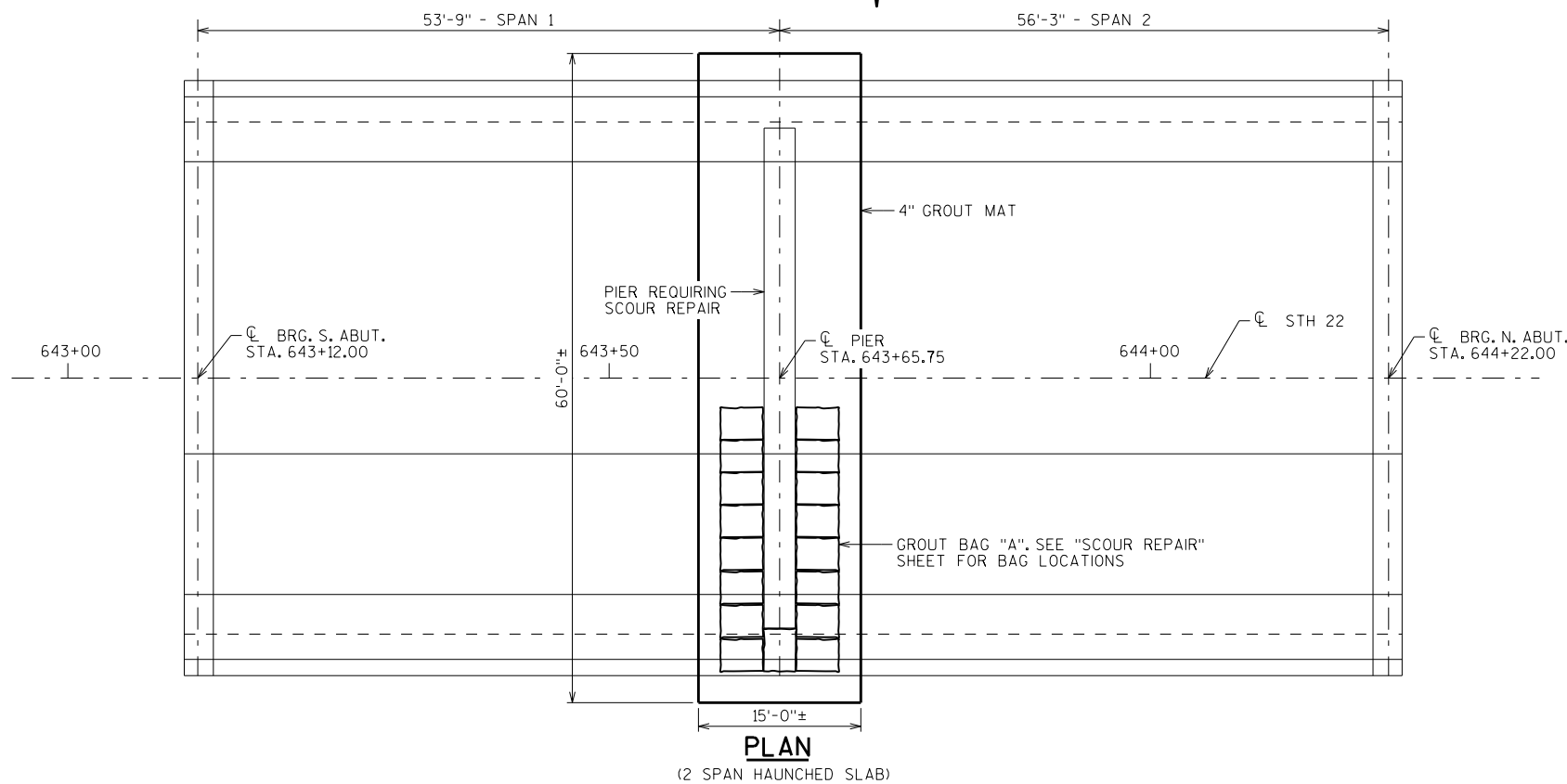
1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/s/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



CROSS SECTION THRU ROADWAY @ PIER
(LOOKING NORTH)



PLAN
(2 SPAN HAUNCHED SLAB)

DESIGN DATA

LIVE LOAD: (RATINGS TAKEN FROM HSI, 05/24/2013)
 DESIGN LOADING: HS-20
 INVENTORY RATING: HS-20
 OPERATING RATING: HS-33
 MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 (KIPS)
 ULTIMATE DESIGN STRESSES:
 CONCRETE MASONRY GROUT — F'C = 3,000 P.S.I.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED
 DIMENSIONS SHOWN ON THE PLANS ARE BASED
 ON THE ORIGINAL STRUCTURE PLANS

NOTES

- RIVER BOTTOM ELEVATION AND EXTENT OF SCOUR AND UNDERMINING OF PIER SHAFT ARE FROM THE "UNDERWATER BRIDGE INSPECTION REPORT S.T.H. 22 BRIDGE OVER THE FOX RIVER, STRUCTURE B-39-49" DATED 10/13/2016 BY COLLINS ENGINEERS.
- REMOVE SILT AND LOOSE MATERIAL UNDER PIER SHAFT BEFORE PLACING GROUT MAT, BAGS AND GROUT, WORK TO BE INCIDENTAL TO BID ITEM "SCOUR REPAIR, GROUT MATS 4" ".
- REMOVE MATERIAL THAT CONFLICTS WITH OR COULD DAMAGE GROUT MAT.
- GROUT BAG "A" SHALL BE A MINIMUM OF 3'-0" WIDE X 4'-0" LONG X 1'-0" THICK, AND A MAXIMUM OF 3'-6" WIDE X 8'-0" LONG X 2'-6" THICK.
- REMOVE OR CUT GROUT TUBE FLUSH WITH TOP OF BAGS AFTER FILLING CAVITY BELOW PIER SHAFT WITH GROUT.
- GROUT BAGS AND MAT ARE TO BE PLACED SO THAT THERE IS NO GAP BETWEEN THE BAGS AND THE PIER SHAFT. PIER SHAFT MAY HAVE FORMLINER SURFACE.
- ADJACENT MATS SHALL BE JOINED BY FIELD SEWING OR ZIPPING BEFORE FILLING THE MATS WITH GROUT, ALTERNATE DETAIL IS ALLOWED AS SHOWN ON THE PLANS.
- STACK GROUT BAGS AS REQUIRED TO COVER VOIDS BY 6" MIN. JOINTS BETWEEN BAGS IN SUCCESSIVE ROWS AND TIERS SHALL BE STAGGERED. PIN ROWS TOGETHER WITH #5 BARS @ 4'-0" SPACING.
- LIMITS OF GROUT MAT ARE DIMENSIONED AND MEASURED ALONG FINISHED SURFACE OF MAT AFTER GROUTING.
- GROUT MAT SHOULD BE TOPPED OFF WITH NATIVE LOCAL ROUNDED FIELD STONE. TO BE PAID FOR UNDER "RIPRAP LIGHT".

BENCH MARK

NO.	LOCATION	DESCRIPTION	ELEV.
1	PIER 1	TOP WALL AT UPSTREAM (WEST) NOSE	773.60

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTAL
606.0100	RIPRAP LIGHT	CY	30
SPV.0035	SCOUR REPAIR GROUT	CY	3
SPV.0035	SCOUR REPAIR, GROUT BAGS	CY	11
SPV.0035	SCOUR REPAIR, GROUT MATS 4"	CY	10

LIST OF DRAWINGS

- GENERAL PLAN
- SCOUR REPAIR

STRUCTURE DESIGN CONTACT:
 ANTHONY P. LANDINI (608) 266-7818
 LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY

BUREAU OF STRUCTURES

ACCEPTED *William C. Dreher* 2/16/18
 CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-39-49

STH 22 OVER THE FOX RIVER

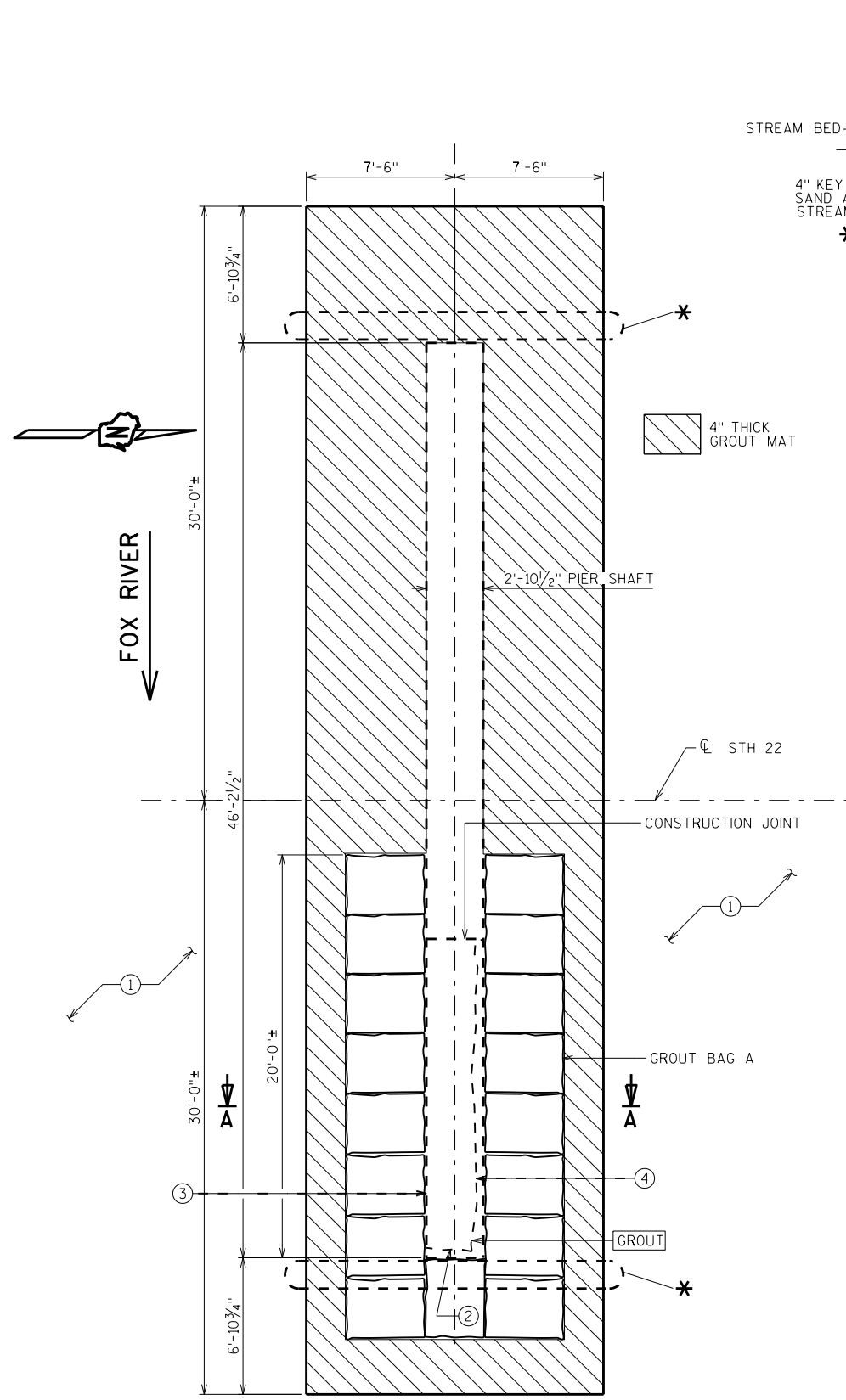
COUNTY MARQUETTE TOWN/CITY/VILLAGE MONTELLLO

DESIGN SPEC. REHABILITATION

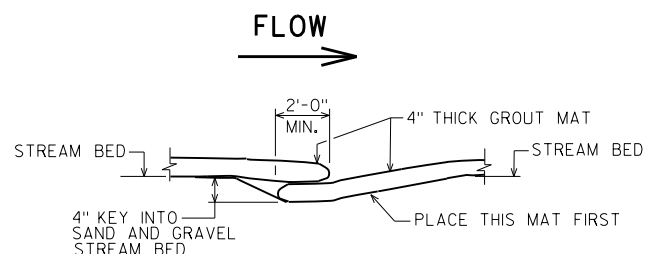
DESIGNED BY APL DESIGNED CK'D. MJL DRAWN BY MJL PLANS CK'D. APL

GENERAL PLAN

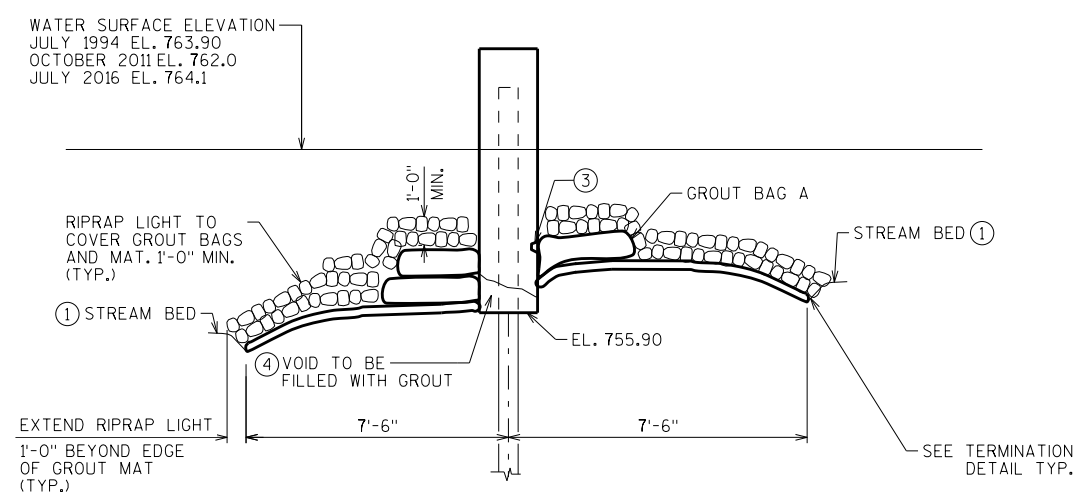
SHEET 1 OF 2



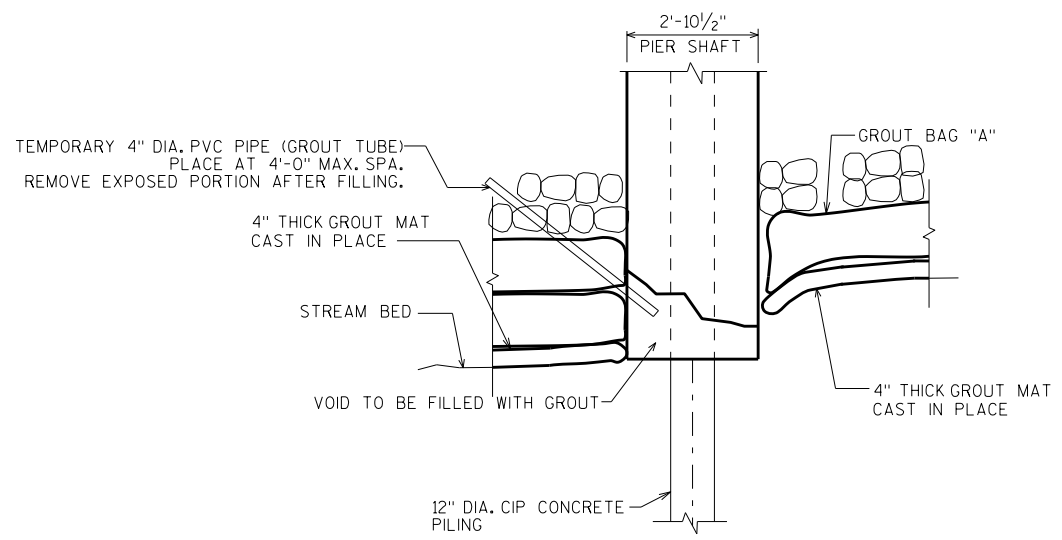
PLAN @ PIER



***ALTERNATE JOINING DETAIL**
 INSTEAD OF SEWING OR ZIPPERING, ADJACENT MATS MAY BE OVERLAPPED AT SHOWN LOCATIONS ONLY. ALL MATERIALS AND WORK IN BOTTOM OVERLAP IS CONSIDERED INCIDENTAL TO BID ITEM, SCOUR REPAIR, GROUT MATS 4".

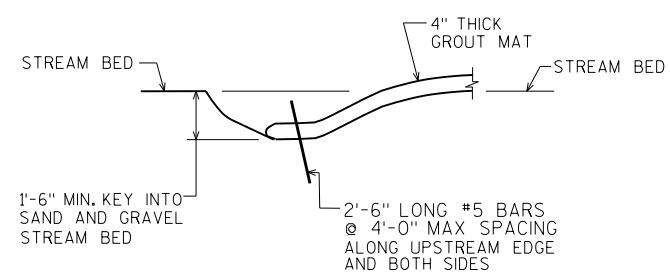


SECTION A-A



TYPICAL SECTION

LOOKING EAST



TERMINATION DETAIL

RIPRAP NOT SHOWN FOR CLARITY

- ① CHANNEL BOTTOM MATERIAL CONSISTS OF 6 INCH DIAMETER AND SMALLER STONES.
- ② 3 FT WIDE BY 6 INCH HIGH VOID WITH 3 INCH MAXIMUM PENETRATION INTO PIER CONCRETE.
- ③ INTERMITTENT 3 INCH HIGH VOIDS WITH 3 INCH MAXIMUM PENETRATION INTO PIER CONCRETE FROM THE DOWNSTREAM NOSE EXTENDING 5 FEET TO THE WEST.
- ④ 16 FT WIDE BY 15 FT HIGH VOID WITH 3 FT MAXIMUM PENETRATION INTO PIER CONCRETE.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-39-49			
		DRAWN BY	PLANS CK'D.
		MJL	APL
SCOUR REPAIR			SHEET 2

DESIGN DATA

LIVE LOAD: (RATINGS TAKEN FROM HSI, 05/24/2013)
 DESIGN LOADING: HS-20
 INVENTORY RATING: HS-23
 OPERATING RATING: HS-38
 MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 (KIPS)

ULTIMATE DESIGN STRESSES:
 CONCRETE MASONRY GROUT — F'c = 3,000 P.S.I.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED
 DIMENSIONS SHOWN ON THE PLANS ARE BASED ON THE ORIGINAL STRUCTURE PLANS

NOTES

RIVER BOTTOM ELEVATION AND EXTENT OF SCOUR AND UNDERMINING OF PIER SHAFT ARE FROM THE "UNDERWATER BRIDGE INSPECTION REPORT S.T.H. 22 BRIDGE OVER THE MONTELLO RIVER, STRUCTURE B-39-51" DATED 10/13/2016 BY COLLINS ENGINEERS.

REMOVE SILT AND LOOSE MATERIAL UNDER PIER SHAFT BEFORE PLACING GROUT MAT, BAGS AND GROUT, WORK TO BE INCIDENTAL TO BID ITEM "SCOUR REPAIR, GROUT MATS 4" ".

REMOVE MATERIAL THAT CONFLICTS WITH OR COULD DAMAGE GROUT MAT.

GROUT BAG "A" SHALL BE A MINIMUM OF 3'-0" WIDE X 4'-0" LONG X 1'-0" THICK, AND A MAXIMUM OF 3'-6" WIDE X 8'-0" LONG X 2'-6" THICK.

REMOVE OR CUT GROUT TUBE FLUSH WITH TOP OF BAGS AFTER FILLING CAVITY BELOW PIER SHAFT WITH GROUT.

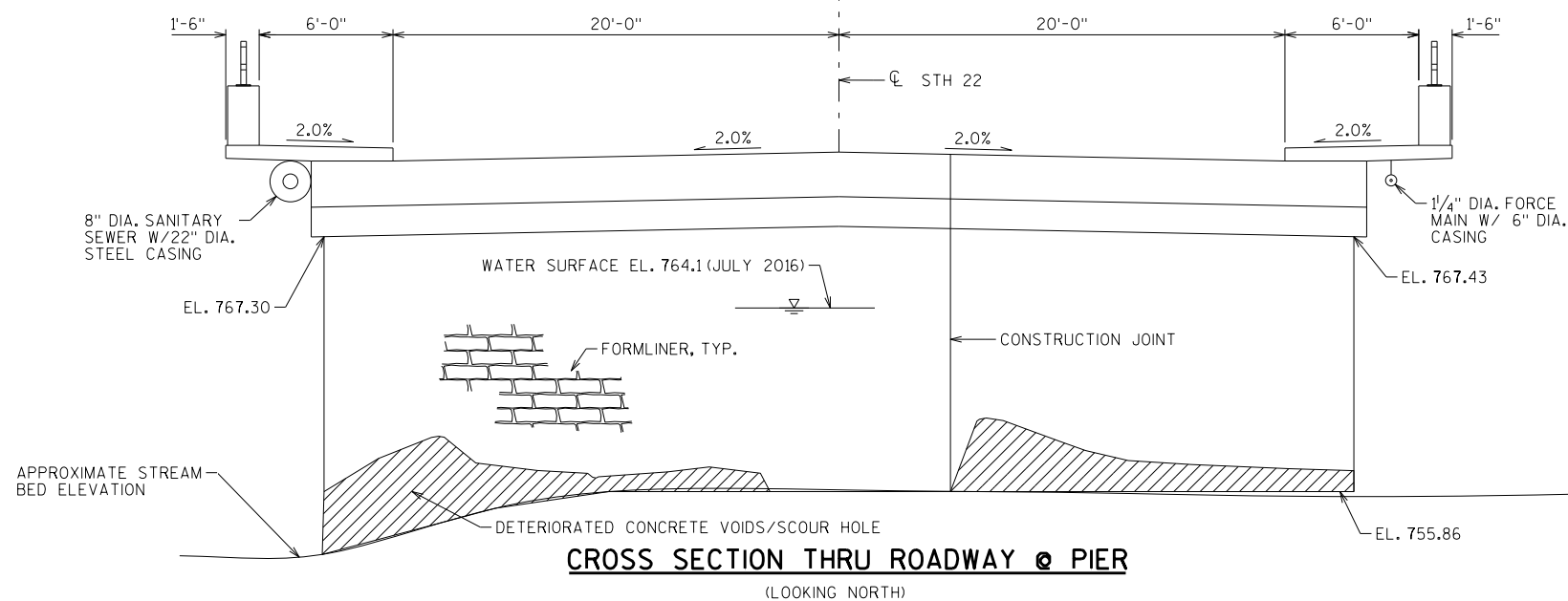
GROUT BAGS AND MAT ARE TO BE PLACED SO THAT THERE IS NO GAP BETWEEN THE BAGS AND THE PIER SHAFT. PIER SHAFT MAY HAVE FORMLINER SURFACE.

ADJACENT MATS SHALL BE JOINED BY FIELD SEWING OR ZIPPERING BEFORE FILLING THE MATS WITH GROUT, ALTERNATE DETAIL IS ALLOWED AS SHOWN ON THE PLANS.

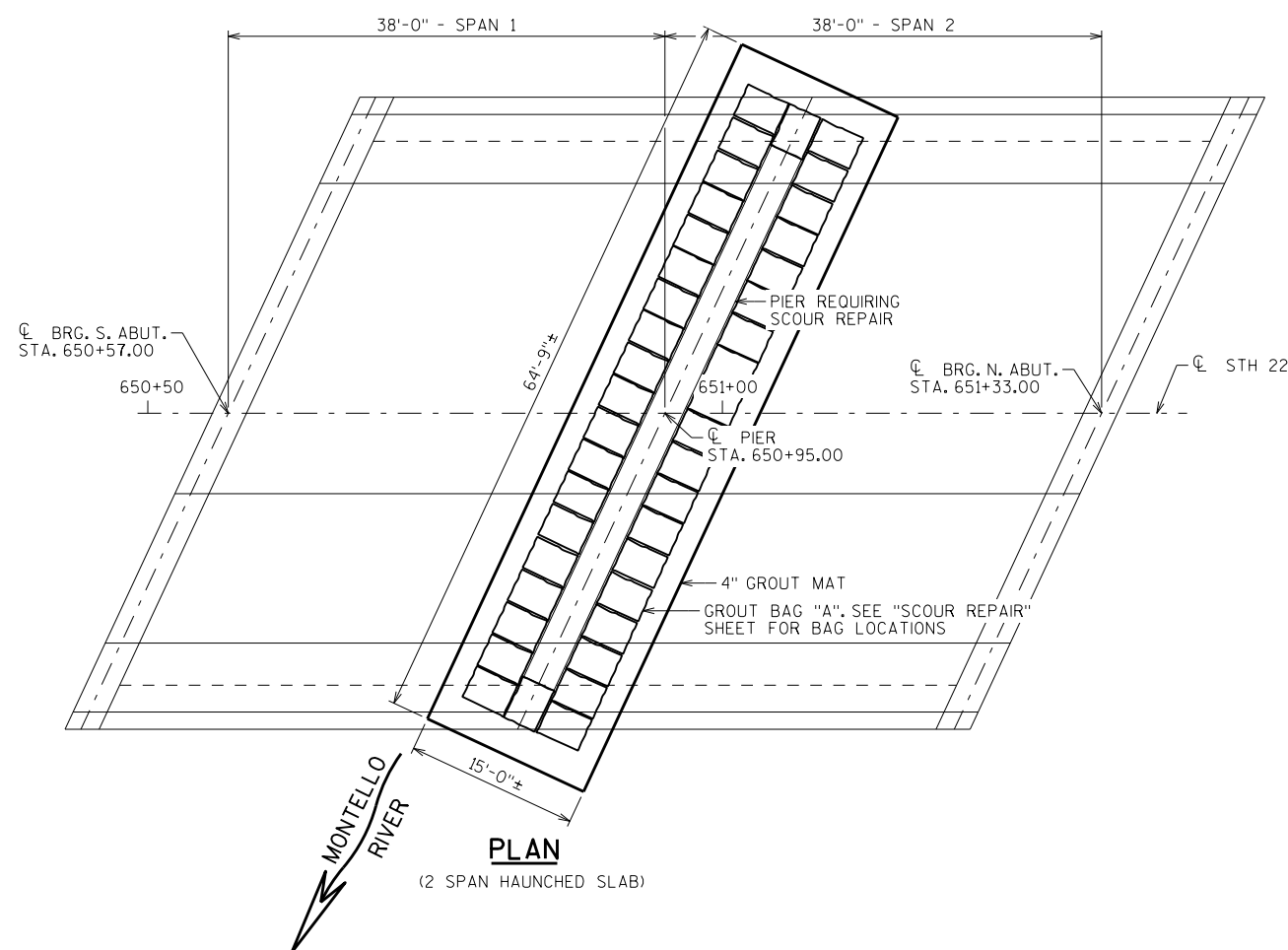
STACK GROUT BAGS AS REQUIRED TO COVER VOIDS BY 6" MIN. JOINTS BETWEEN BAGS IN SUCCESSIVE ROWS AND TIERS SHALL BE STAGGERED. PIN ROWS TOGETHER WITH #5 BARS @ 4'-0" SPACING.

LIMITS OF GROUT MAT ARE DIMENSIONED AND MEASURED ALONG FINISHED SURFACE OF MAT AFTER GROUTING.

GROUT MAT SHOULD BE TOPPED OFF WITH NATIVE LOCAL ROUNDED FIELD STONE, TO BE PAID FOR UNDER "RIPRAP LIGHT".



CROSS SECTION THRU ROADWAY @ PIER
 (LOOKING NORTH)



PLAN
 (2 SPAN HAUNCHED SLAB)

BENCH MARK

NO.	LOCATION	DESCRIPTION	ELEV.
1	PIER 1	TOP WALL AT UPSTREAM (WEST) NOSE	767.30

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTAL
606.0100	RIPRAP LIGHT	CY	33
SPV.0035	SCOUR REPAIR GROUT	CY	12
SPV.0035	SCOUR REPAIR, GROUT BAGS	CY	26
SPV.0035	SCOUR REPAIR, GROUT MATS 4"	CY	11

LIST OF DRAWINGS

- GENERAL PLAN
- SCOUR REPAIR

STRUCTURE DESIGN CONTACT:
 MICHAEL LARSON (608) 267-4539
 LAURA SHADEWALD (608) 267-9592

NO.	DATE	REVISION	BY

BUREAU OF STRUCTURES
 WISCONSIN DEPARTMENT OF TRANSPORTATION

ACCEPTED *William C. Dreher* 2/16/18
 CHIEF STRUCTURES DESIGN ENGINEER DATE

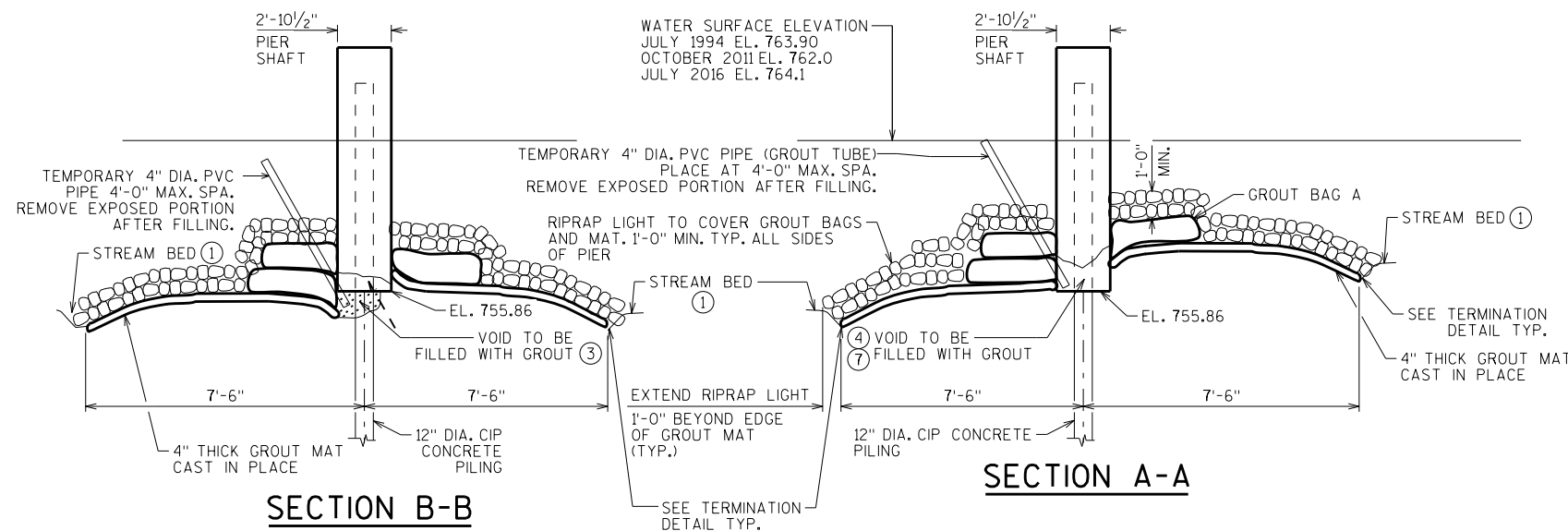
STRUCTURE B-39-51
 STH 22 OVER THE MONTELLO RIVER

COUNTY MARQUETTE TOWN/CITY/VILLAGE MONTELLO

DESIGN SPEC. REHABILITATION

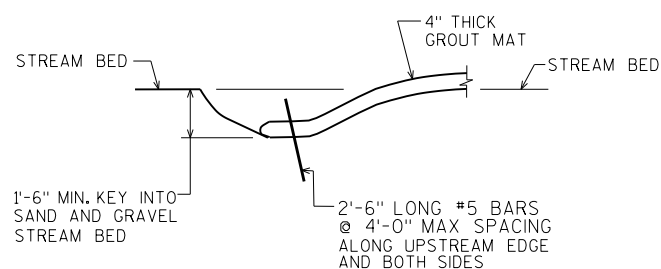
DESIGNED BY M.J.L. DESIGNED CK'D. APL DRAWN BY M.J.L. PLANS CK'D. APL

GENERAL PLAN SHEET 1 OF 2



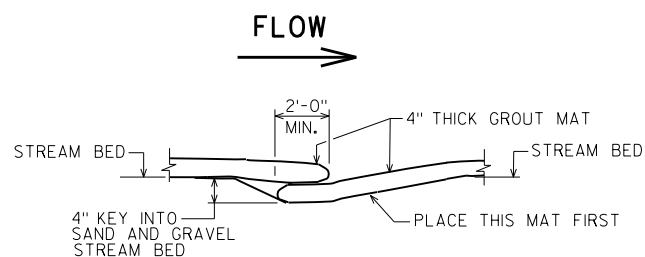
SECTION A-A

SECTION B-B



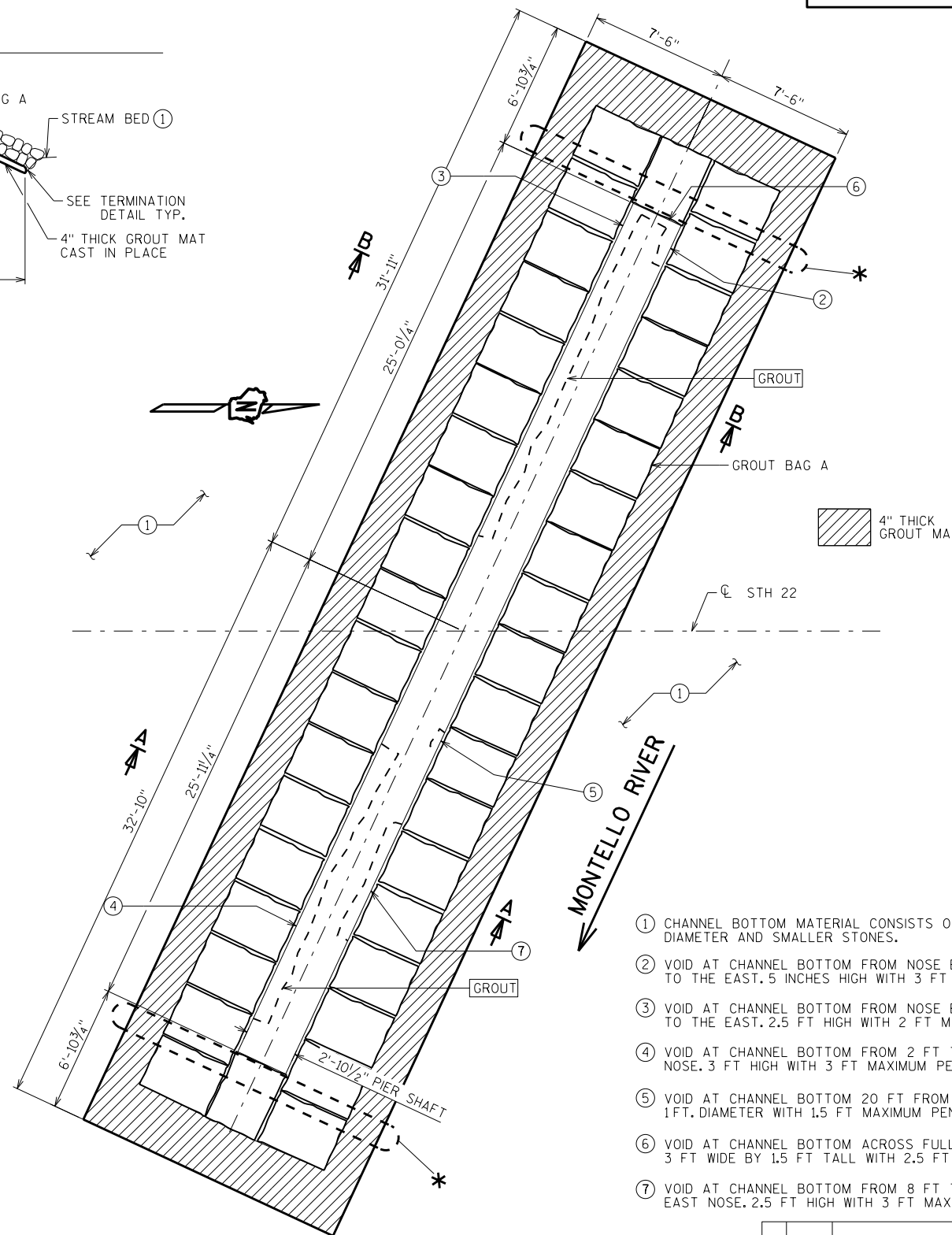
TERMINATION DETAIL

RIPRAP NOT SHOWN FOR CLARITY



*ALTERNATE JOINING DETAIL

INSTEAD OF SEWING OR ZIPPERING, ADJACENT MATS MAY BE OVERLAPPED AT SHOWN LOCATIONS ONLY. ALL MATERIALS AND WORK IN BOTTOM OVERLAP IS CONSIDERED INCIDENTAL TO BID ITEM, SCOUR REPAIR, GROUT MATS 4".

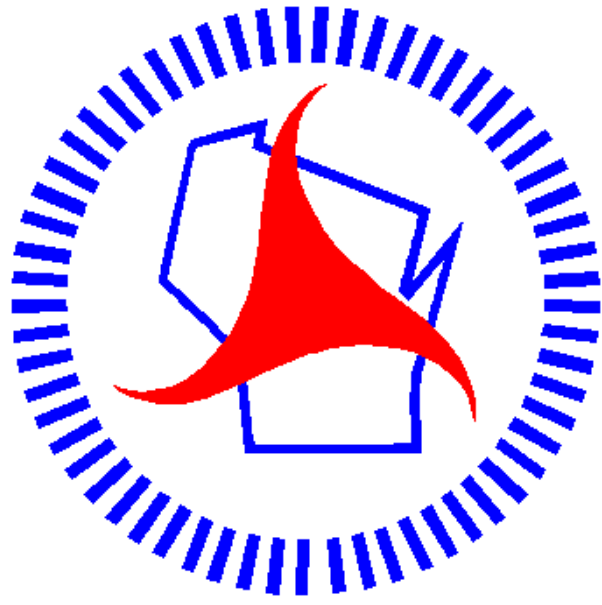


PLAN @ PIER

- ① CHANNEL BOTTOM MATERIAL CONSISTS OF 6 INCH DIAMETER AND SMALLER STONES.
- ② VOID AT CHANNEL BOTTOM FROM NOSE EXTENDING 2.5 FT TO THE EAST. 5 INCHES HIGH WITH 3 FT MAXIMUM PENETRATION.
- ③ VOID AT CHANNEL BOTTOM FROM NOSE EXTENDING 20 FT TO THE EAST. 2.5 FT HIGH WITH 2 FT MAXIMUM PENETRATION.
- ④ VOID AT CHANNEL BOTTOM FROM 2 FT TO 18.5 FT FROM NOSE. 3 FT HIGH WITH 3 FT MAXIMUM PENETRATION.
- ⑤ VOID AT CHANNEL BOTTOM 20 FT FROM EAST NOSE. 1 FT. DIAMETER WITH 1.5 FT MAXIMUM PENETRATION.
- ⑥ VOID AT CHANNEL BOTTOM ACROSS FULL WIDTH OF NOSE. 3 FT WIDE BY 1.5 FT TALL WITH 2.5 FT MAXIMUM PENETRATION.
- ⑦ VOID AT CHANNEL BOTTOM FROM 8 FT TO 15 FT FROM EAST NOSE. 2.5 FT HIGH WITH 3 FT MAXIMUM PENETRATION.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-39-51			
DRAWN BY		MJL	PLANS CK'D. APL
SCOUR REPAIR			SHEET 2

Notes



Wisconsin Department of Transportation

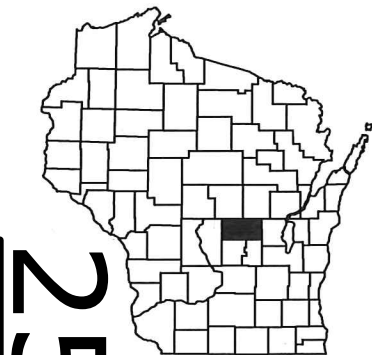
Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 14



25

DESIGN DESIGNATION

A.A.D.T. 2018	=	5900
A.A.D.T. 2028	=	6300
D.H.V.	=	733
D.D.	=	6040
T.	=	15.3
DESIGN SPEED	=	60 MPH
ESALS	=	

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

WAUTOMA - REDGRANITE

CTH Z - CTH S

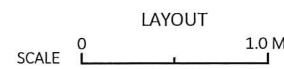
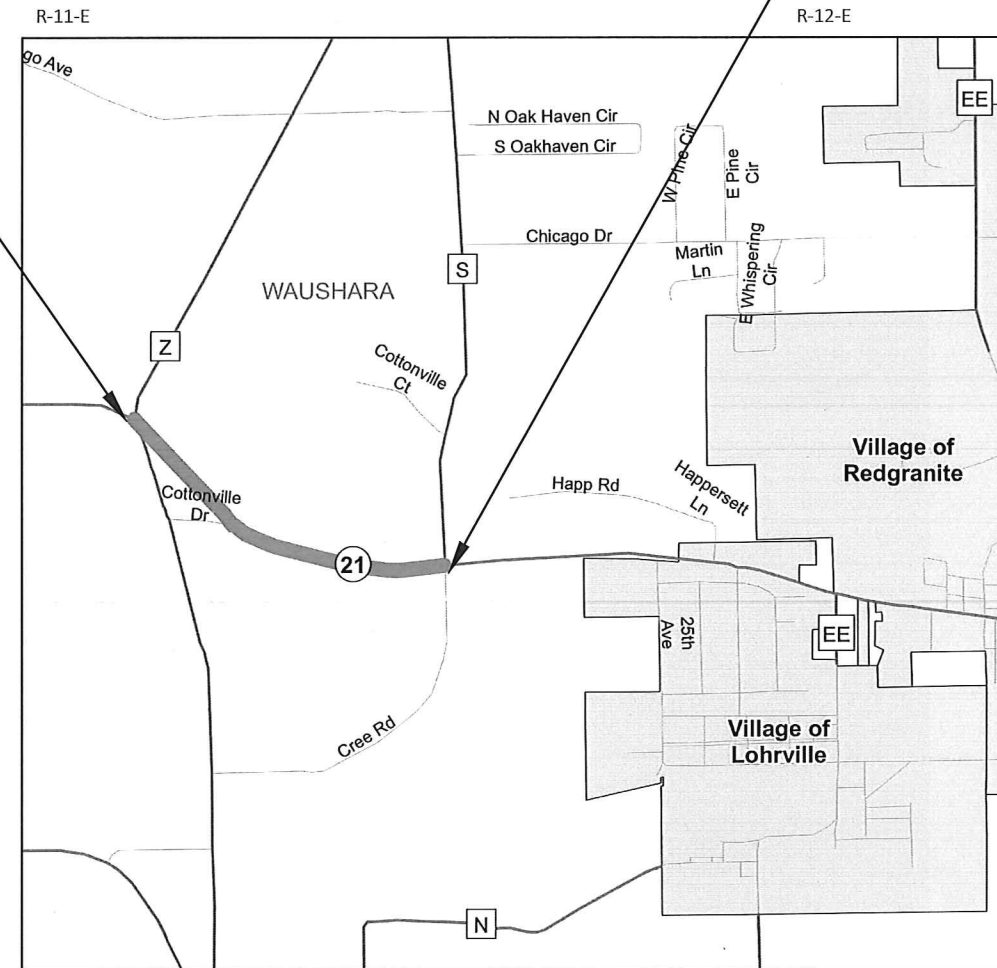
STH 21

WAUSHARA

STATE PROJECT NUMBER
6180-00-72

START PROJECT
STA 347+00

END PROJECT
STA 421+00



TOTAL NET LENGTH OF CENTERLINE = 1.25 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, WAUSHARA COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES. ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 NAVD 88 (2012).

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6180-00-72	WISC 2018252	1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	NC REGION
Designer	JASON SCHAFFER
Project Manager	DAN HOLLOWAY
Regional Examiner	CHERYL SIMON
Regional Supervisor	NICHOLE LYSNE

APPROVED FOR THE DEPARTMENT

DATE: 1/29/18


(Signature)

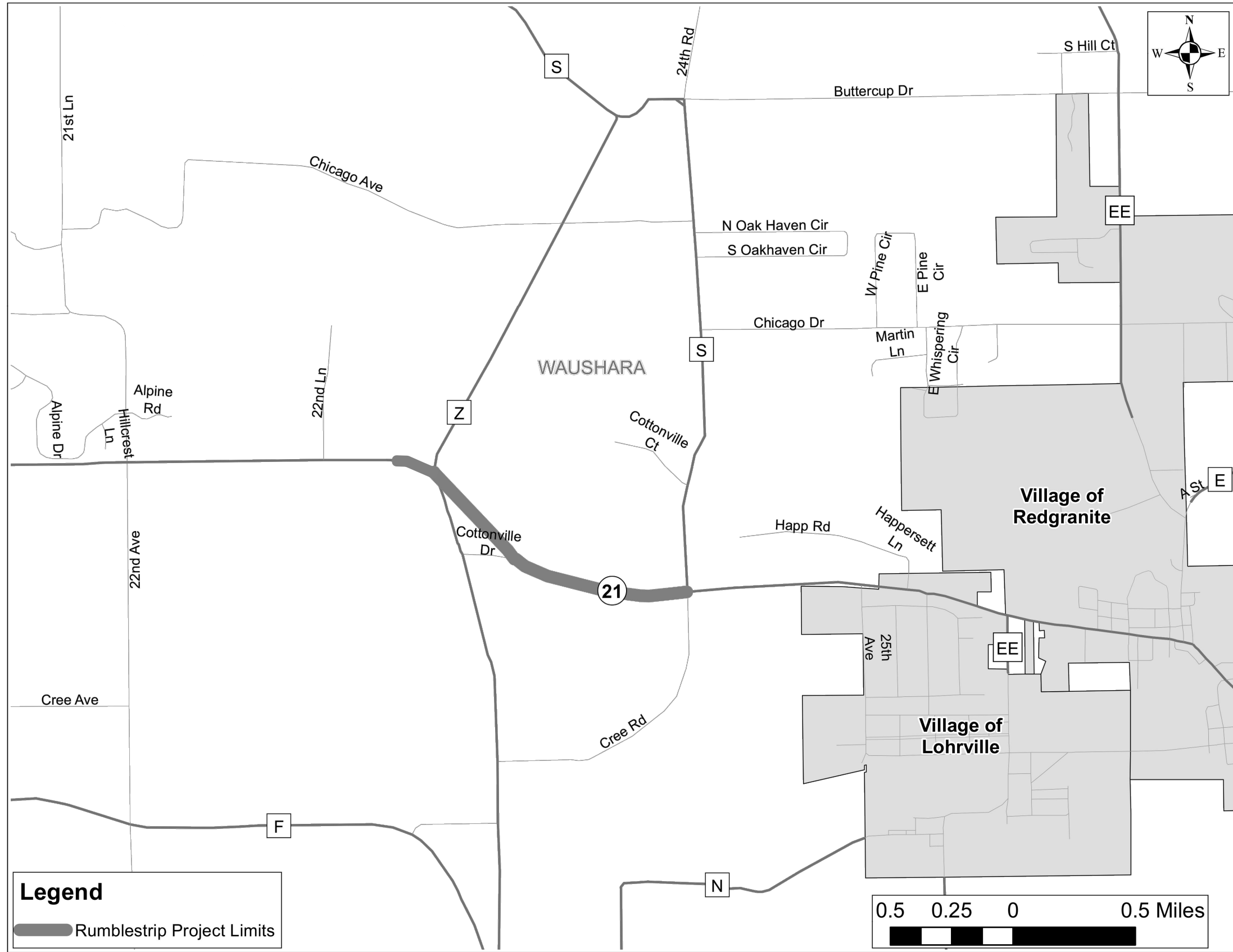
GENERAL NOTES

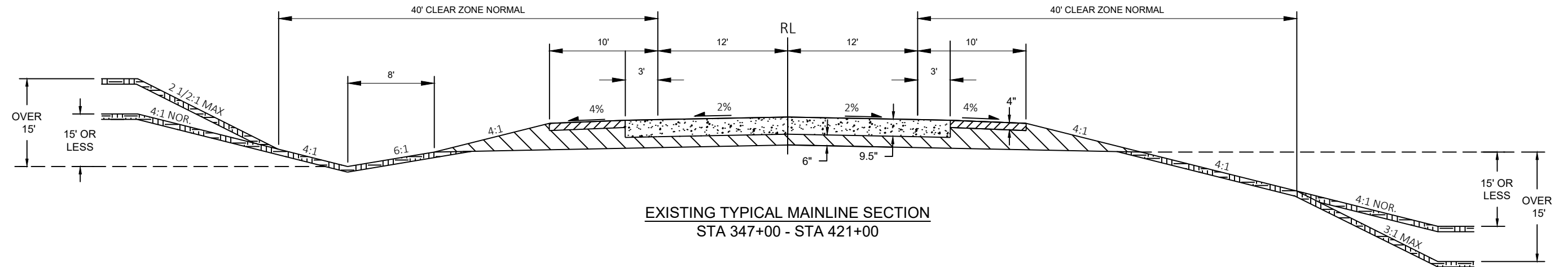
THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS.

Bobbi Jo Fischer
Environmental Analysis Supervisor
Wisconsin Department of Natral Resources
473 Griffith Avenue
Wisconsin Rapids, WI 54494
Phone: 715-421-7845
Bobbi.fischer@wisconsin.gov







Dial  or (800)242-8511
www.DiggersHotline.com

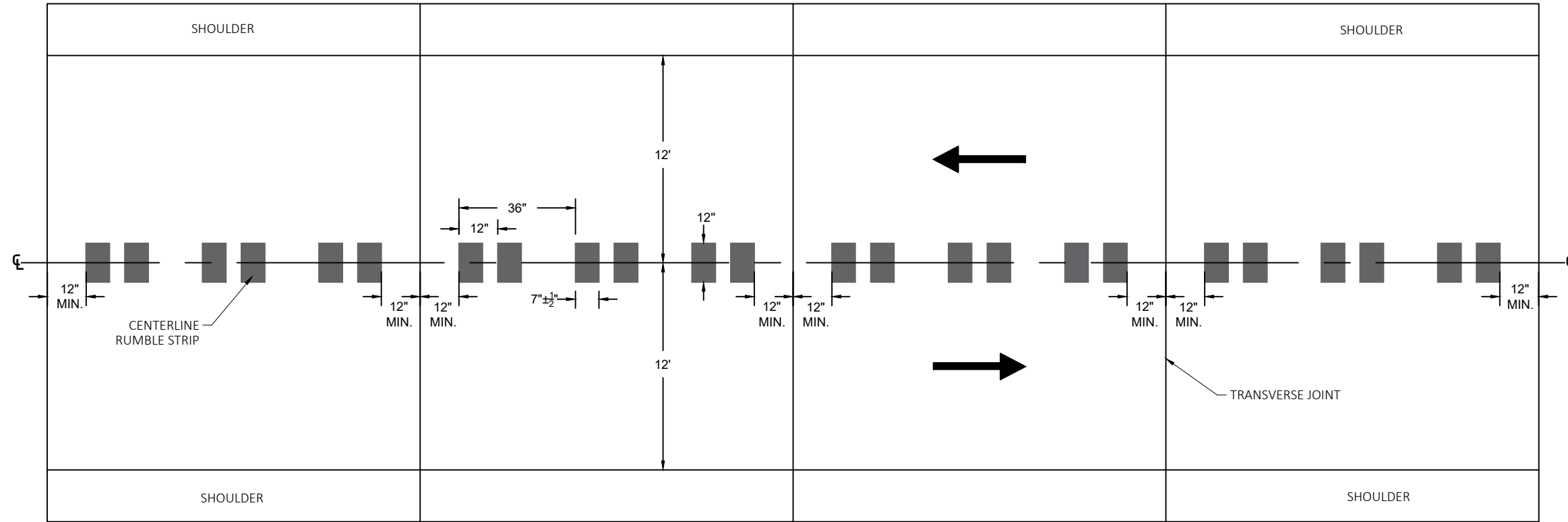




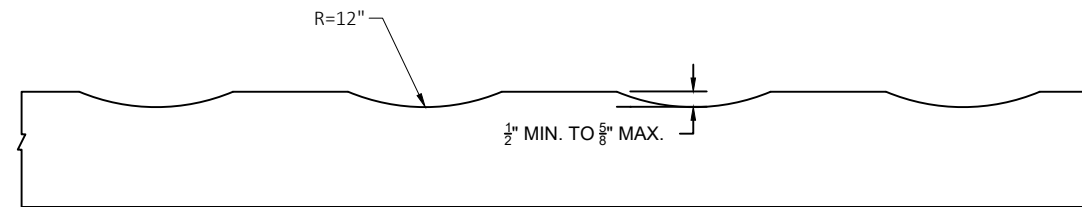
LEGEND

-  CONCRETE PAVEMENT
-  BASE AGG DENSE 1 1/4-INCH
-  BASE AGG DENSE 3/4-INCH
-  ORIGINAL GROUND

- * NUMBER OF CENTERLINE RUMBLE STRIPS IN BETWEEN JOINTS MAY VARY
- * OMIT CENTER LINE RUMBLE STRIPS 75' EITHER SIDE OF CENTER OF COUNTY Z INTERSECTION



PLAN



PROFILE

Estimate Of Quantities By Plan Sets

6180-00-72

Line	Item	Item Description	Unit	Total	Qty
0018	213.0100	Finishing Roadway (project) 03. 6180-00-72	EACH	1.000	1.000
0098	619.1000	Mobilization	EACH	0.200	0.200
0136	643.0310.S	Temporary Portable Rumble Strips	LS	1.000	1.000
0150	643.5000	Traffic Control	EACH	0.200	0.200
0156	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	11,200.000	11,200.000
0230	SPV.0090	Special 01. Concrete Center Line Rumble Strip 2-Lane Rural	LF	7,400.000	7,400.000

FINISHING ROADWAY (PROJECT)

STATION - STATION	LOCATION	213.0100 03. (6180-00-72) EACH
347+00 - 421+00	LT & RT	1
PROJECT TOTAL:		<u>1</u>

MOBILIZATION

PROJECT	619.1000 EACH
6180-00-72	0.20
PROJECT TOTAL:	<u>0.20</u>

TRAFFIC CONTROL

PROJECT	643.5000 EACH
6180-00-72	0.20
PROJECT TOTAL:	<u>0.20</u>

RUMBLE STRIPS

STATION - STATION	SPV.0090.01 CONCRETE CENTER LINE RUMBLE STRIP 2-LANE RURAL LF	643.0310.S TEMPORARY PORTABLE RUMBLE STRIPS EACH
347+00 - 421+00	7400	1
PROJECT TOTALS:	<u>7400</u>	<u>1</u>

MARKING LINE

STATION - STATION	LOCATION	646.4520 SAME DAY EPOXY 4-INCH LF	REMARKS
347+00 - 421+00	STH 21	11200	CENTERLINE
PROJECT TOTAL:		<u>11200</u>	



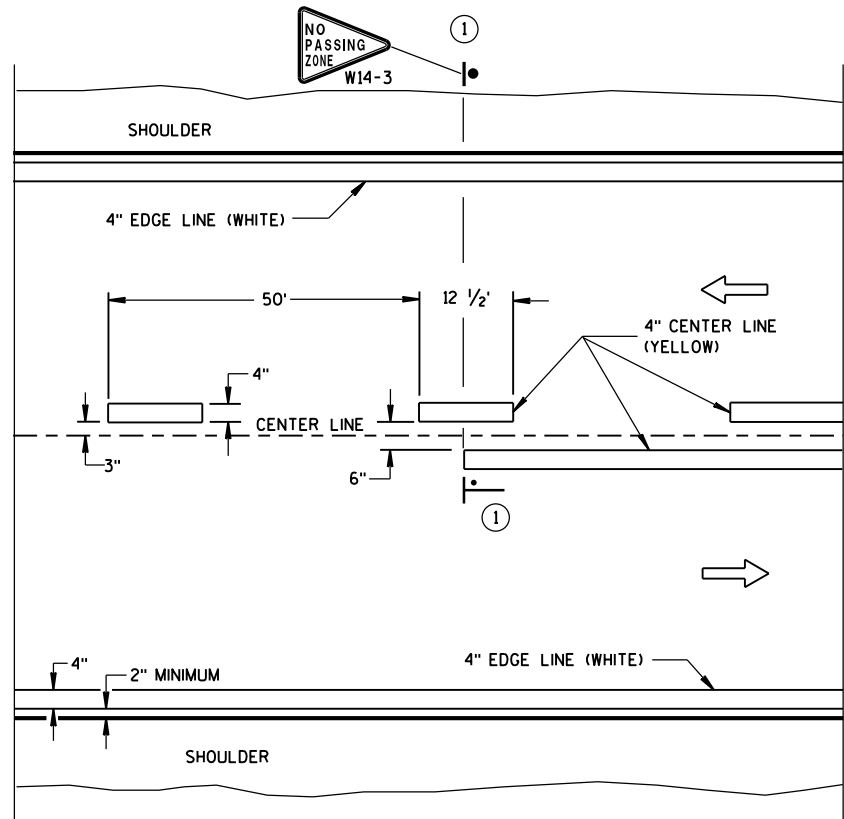
5

5

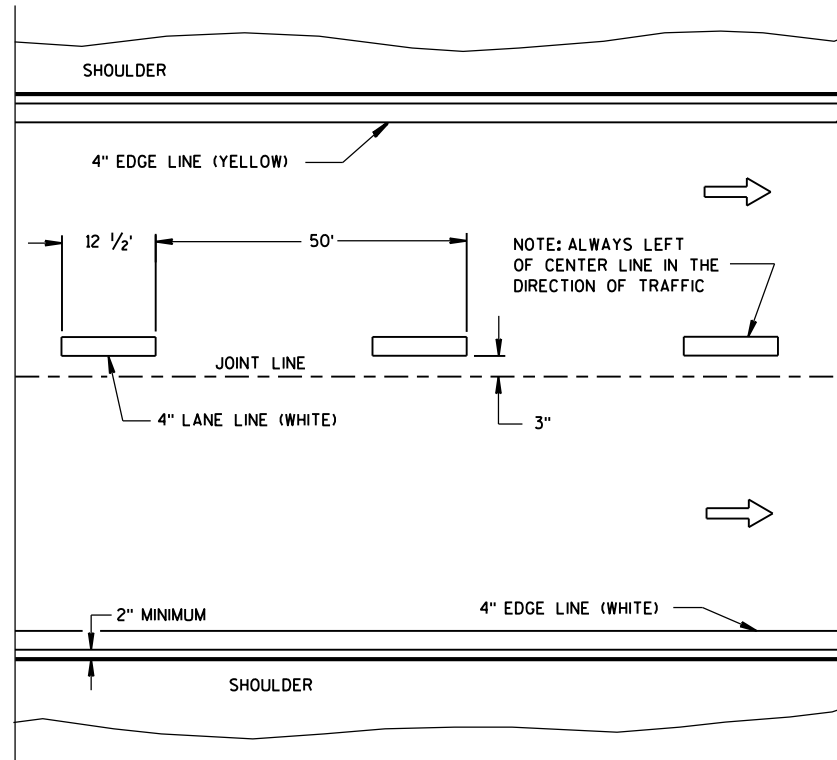
PROJECT NO: 6180-00-72	HWY: STH 21	COUNTY: WAUSHARA	PROJECT OVERVIEW	SHEET	E
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Standard Detail Drawing List

15C08-18A	LONGITUDINAL MARKING (MAINLINE)
15C12-06	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS

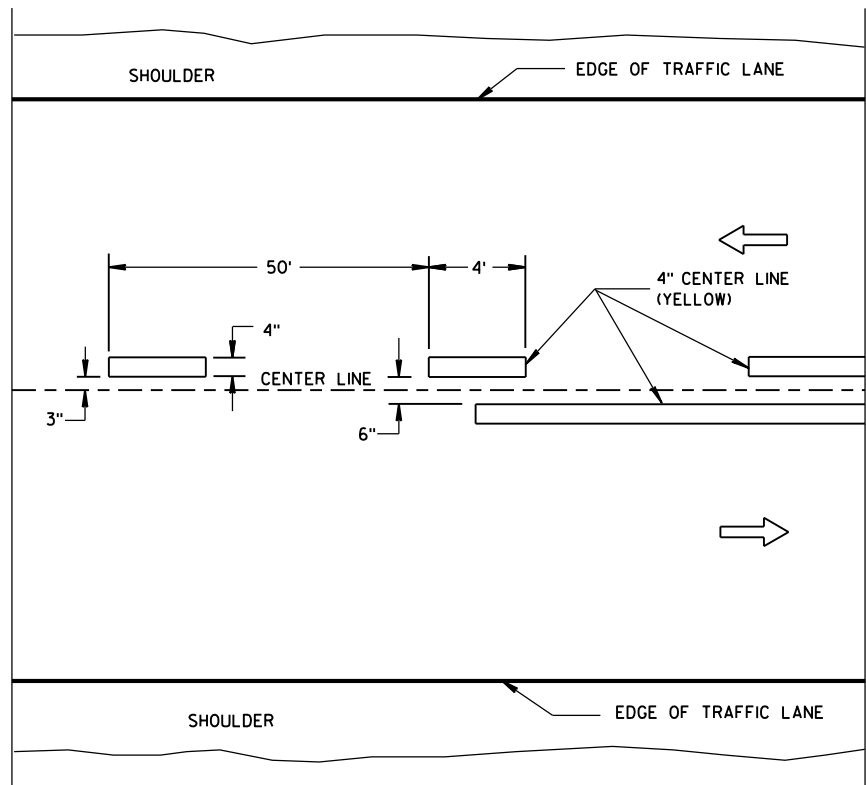


TWO WAY TRAFFIC

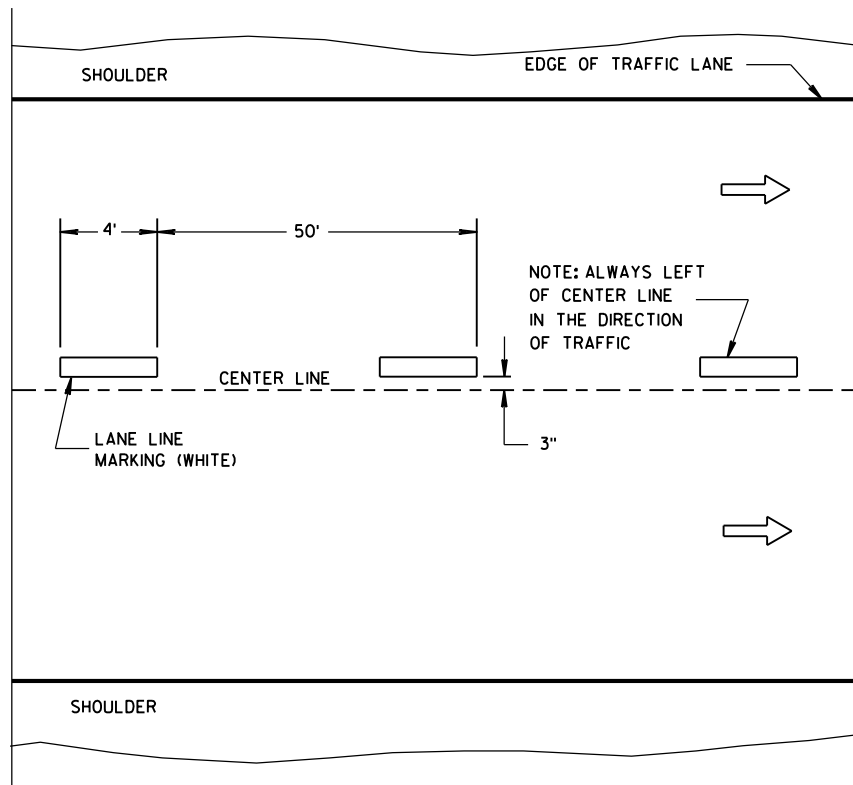


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

LEGEND

- "T" MARKING
- POST MOUNTED SIGN

6





6

S.D.D. 15 C 8-18a

S.D.D. 15 C 8-18a

LONGITUDINAL MARKING (MAINLINE)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017	/S/ Matthew R. Rauch
DATE	STATE SIGNING AND MARKING ENGINEER
FHWA	

LEGEND

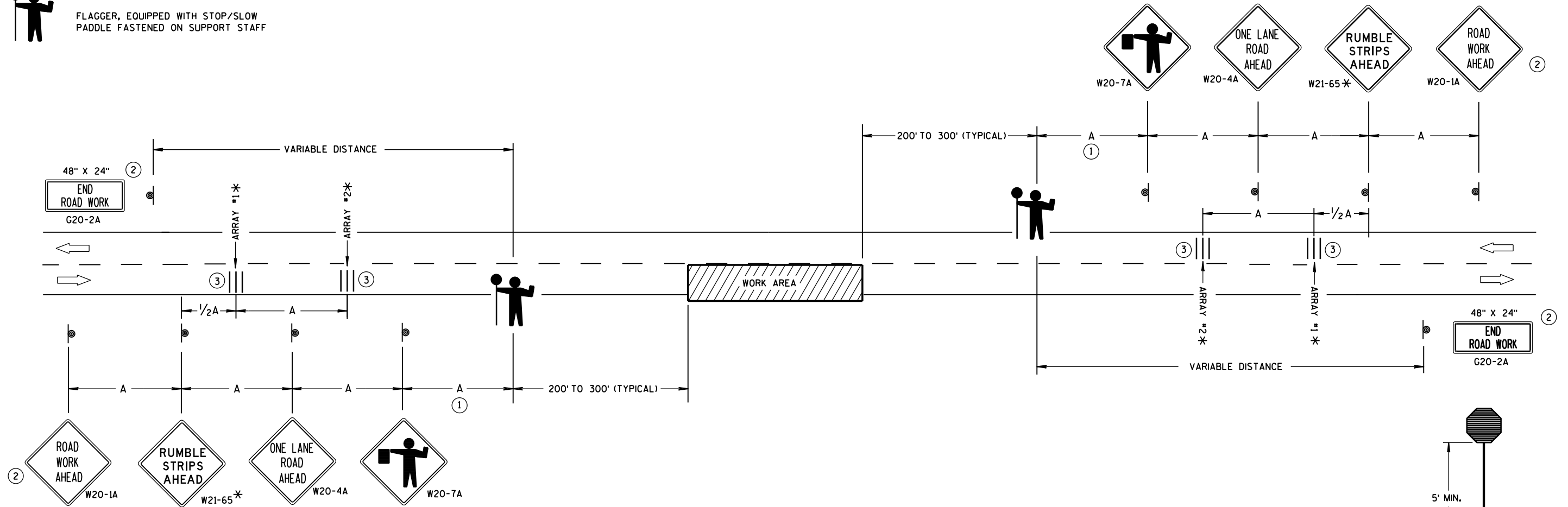
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING A
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING A.



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"W0" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS. PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

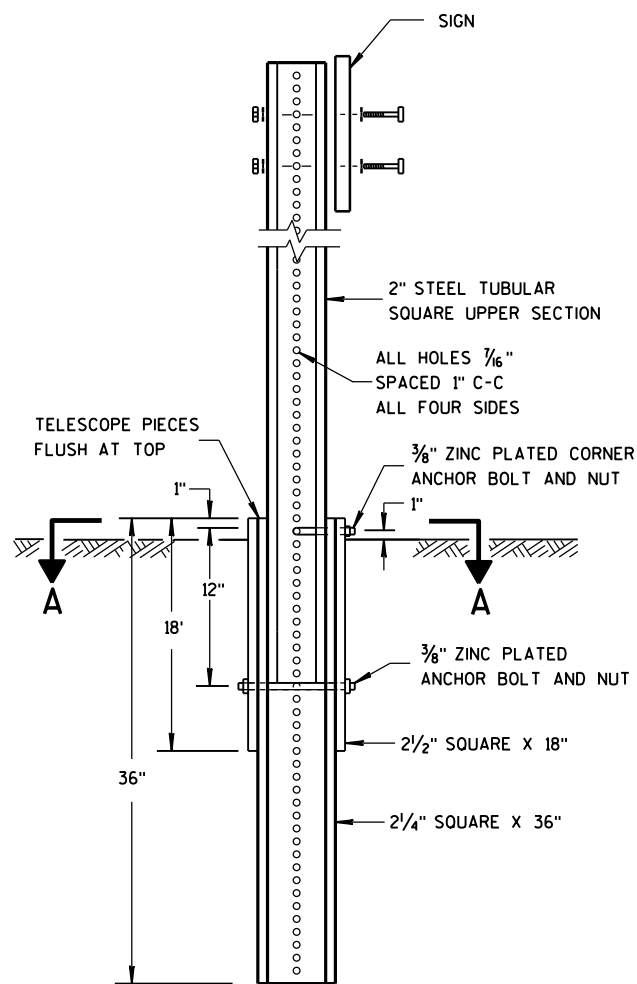
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* UTILIZE TEMPORARY RUMBLE STRIPS WHEN FLAGGING OPERATION IS ANTICIPATED TO BE STATIONARY IN EXCESS OF TWO HOURS.

- ① FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS (IF USED) SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ③ EACH TEMPORARY RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

STOP/SLOW PADDLE ON SUPPORT STAFF

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/S/ Andrew Heldtke WORK ZONE ENGINEER
FHWA	



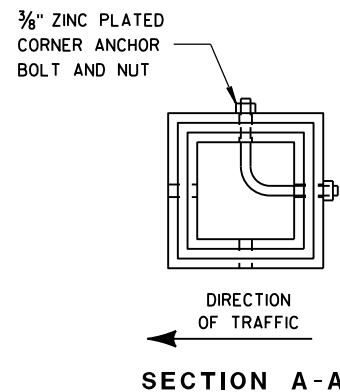
DETAIL OF TUBULAR STEEL SIGN POST

TUBULAR STEEL POSTS

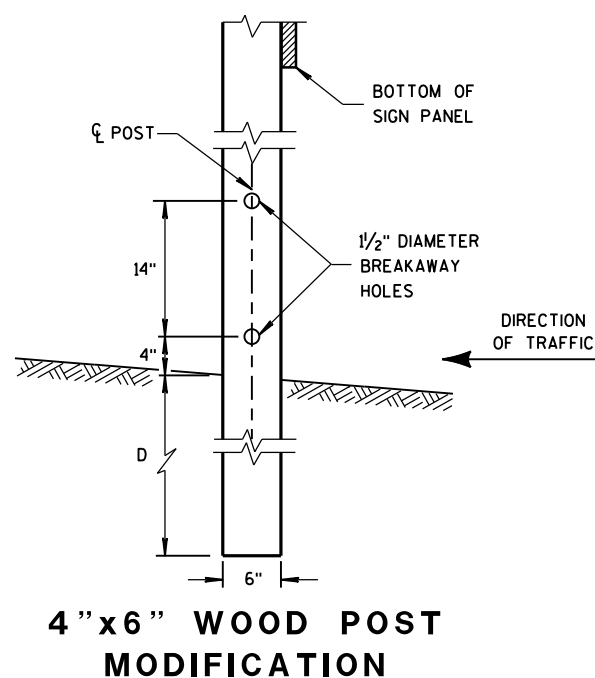
AREA OF SIGN INSTALLATION (SQ. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

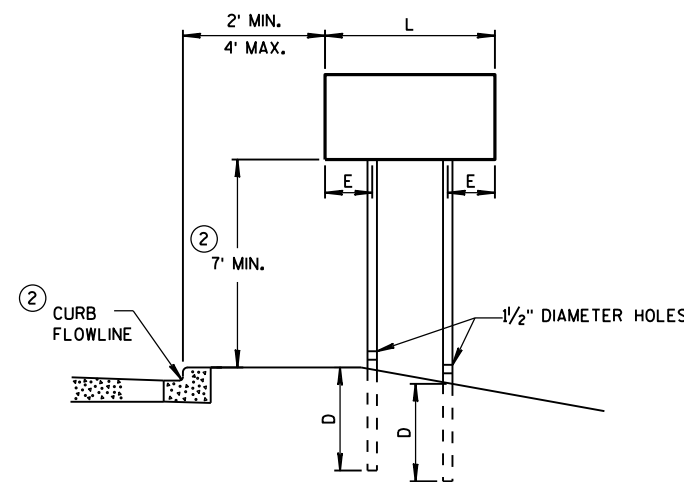


SECTION A-A



GENERAL NOTES

- ① 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.
- ② THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.
- ③ FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.

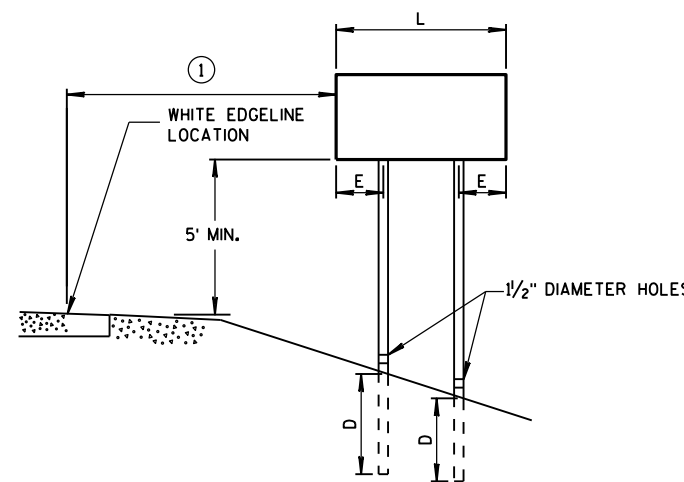


URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST EMBEDMENT DEPTH

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'



RURAL AREA

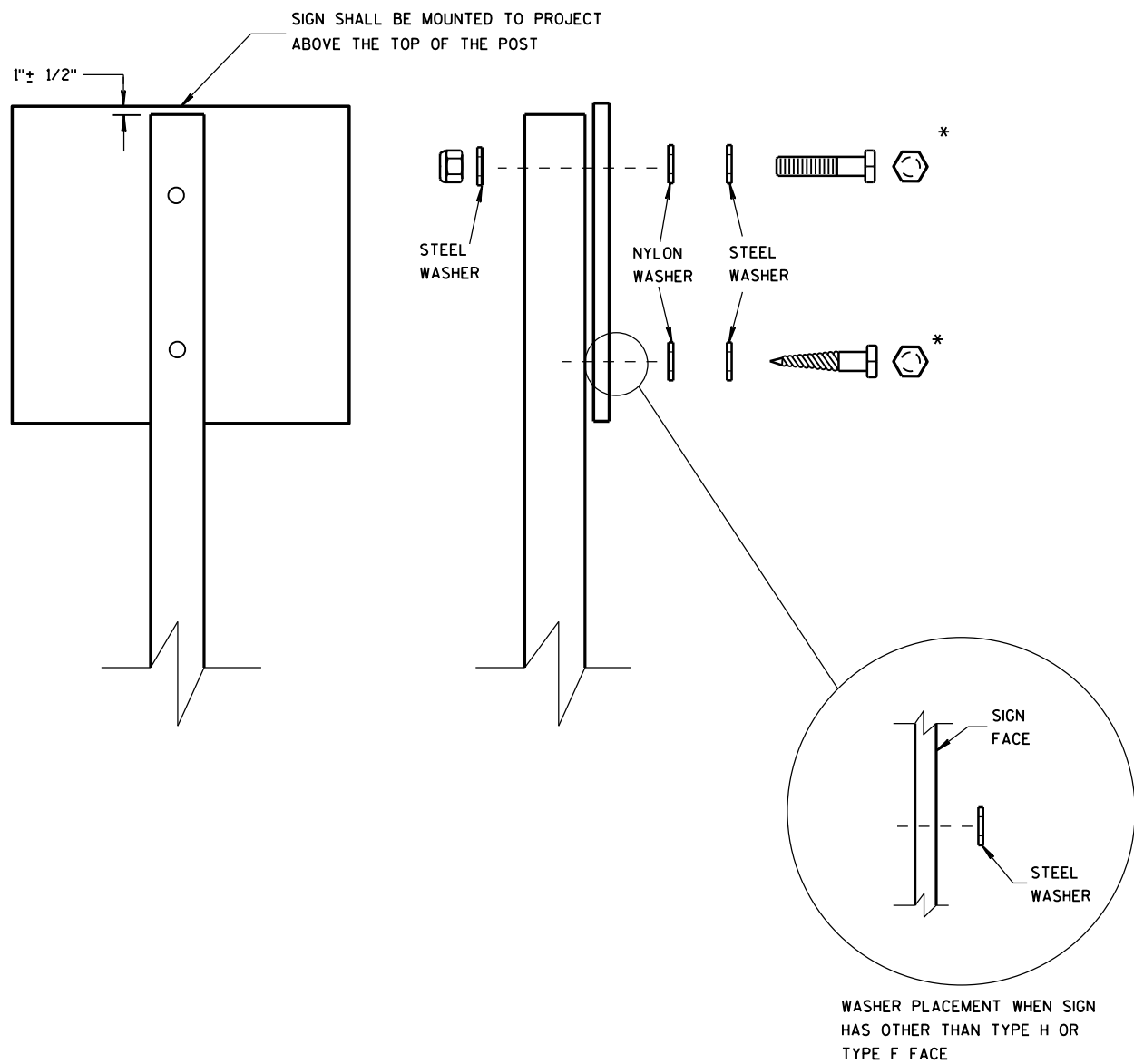
4\"/>

POST SPACING REQUIREMENTS		NUMBER OF WOOD POSTS REQUIRED
L	E	
48\"/>	-	1
LESS THAN 60\"/>	12\"/>	2
60\"/>	L/5	2
GREATER THAN 120\"/>	12\"/>	3
168\"/>	12\"/>	4

SEE NOTE ③

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3

B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3"

MACHINE BOLTS - 5/16" X 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS

RIVETS - 3/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

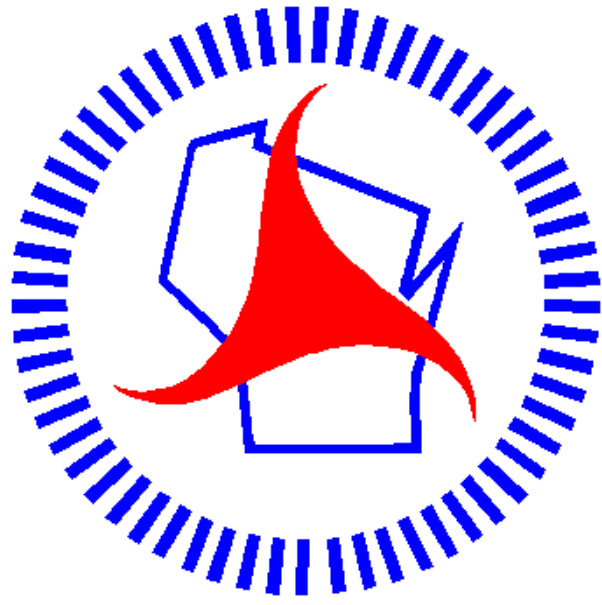
WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

ATTACHMENT OF SIGNS TO POSTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017 DATE	/s/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

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