

HIGHWAY WORK PROPOSAL

Wisconsin Department of Transportation
06/2017 s.66.0901(7) Wis. Stats

Proposal Number: **023**

<u>COUNTY</u>	<u>STATE PROJECT</u>	<u>FEDERAL</u>	<u>PROJECT DESCRIPTION</u>	<u>HIGHWAY</u>
Northwest Region	Wide 1000-18-06	N/A	Sfy 2018 Lil Chip Seals - East; Northwest Region - Various Highways	VAR HWY

This proposal, submitted by the undersigned bidder to the Wisconsin Department of Transportation, is in accordance with the advertised request for proposals. The bidder is to furnish and deliver all materials, and to perform all work for the improvement of the designated project in the time specified, in accordance with the appended Proposal Requirements and Conditions.

Proposal Guaranty Required: \$75,000.00 Payable to: Wisconsin Department of Transportation	Attach Proposal Guaranty on back of this PAGE.
Bid Submittal Date: February 13, 2018 Time (Local Time): 9:00 am	Firm Name, Address, City, State, Zip Code
Contract Completion Time August 31, 2018	SAMPLE NOT FOR BIDDING PURPOSES
Assigned Disadvantaged Business Enterprise Goal 0 %	This contract is exempt from federal oversight.

This certifies that the undersigned bidder, duly sworn, is an authorized representative of the firm named above; that the bidder has examined and carefully prepared the bid from the plans, Highway Work Proposal, and all addenda, and has checked the same in detail before submitting this proposal or bid; and that the bidder or agents, officer, or employees have not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with this proposal bid.

Do not sign, notarize, or submit this Highway Work Proposal when submitting an electronic bid on the Internet.

Subscribed and sworn to before me this date _____

(Signature, Notary Public, State of Wisconsin)

(Bidder Signature)

(Print or Type Name, Notary Public, State Wisconsin)

(Print or Type Bidder Name)

(Date Commission Expires)

(Bidder Title)

Notary Seal

Type of Work: Signs, Pavement Marking, Chip Seal, Scrub Seal, Fog Seal	For Department Use Only
Notice of Award Dated	Date Guaranty Returned

**PLEASE ATTACH
PROPOSAL GUARANTY HERE**

Effective with November 2007 Letting

PROPOSAL REQUIREMENTS AND CONDITIONS

The bidder, signing and submitting this proposal, agrees and declares as a condition thereof, to be bound by the following conditions and requirements.

If the bidder has a corporate relationship with the proposal design engineering company, the bidder declares that it did not obtain any facts, data, or other information related to this proposal from the design engineering company that was not available to all bidders.

The bidder declares that they have carefully examined the site of, and the proposal, plans, specifications and contract forms for the work contemplated, and it is assumed that the bidder has investigated and is satisfied as to the conditions to be encountered, as to the character, quality, and quantities of work to be performed and materials to be furnished, and as to the requirements of the specifications, special provisions and contract. It is mutually agreed that submission of a proposal shall be considered conclusive evidence that the bidder has made such examination.

The bidder submits herewith a proposal guaranty in proper form and amount payable to the party as designated in the advertisement inviting proposals, to be retained by and become the property of the owner of the work in the event the undersigned shall fail to execute the contract and contract bond and return the same to the office of the engineer within fourteen (14) days after having been notified in writing to do so; otherwise to be returned.

The bidder declares that they understand that the estimate of quantities in the attached schedule is approximate only and that the attached quantities may be greater or less in accordance with the specifications.

The bidder agrees to perform the said work, for and in consideration of the payment of the amount becoming due on account of work performed, according to the unit prices bid in the following schedule, and to accept such amounts in full payment of said work.

The bidder declares that all of the said work will be performed at their own proper cost and expense, that they will furnish all necessary materials, labor, tools, machinery, apparatus, and other means of construction in the manner provided in the applicable specifications and the approved plans for the work together with all standard and special designs that may be designed on such plans, and the special provisions in the contract of which this proposal will become a part, if and when accepted. The bidder further agrees that the applicable specifications and all plans and working drawings are made a part hereof, as fully and completely as if attached hereto.

The bidder, if awarded the contract, agrees to begin the work not later than ten (10) days after the date of written notification from the engineer to do so, unless otherwise stipulated in the special provisions.

The bidder declares that if they are awarded the contract, they will execute the contract agreement and begin and complete the work within the time named herein, and they will file a good and sufficient surety bond for the amount of the contract for performance and also for the full amount of the contract for payment.

The bidder, if awarded the contract, shall pay all claims as required by Section 779.14, Statutes of Wisconsin, and shall be subject to and discharge all liabilities for injuries pursuant to Chapter 102 of the Statutes of Wisconsin, and all acts amendatory thereto. They shall further be responsible for any damages to property or injury to persons occurring through their own negligence or that of their employees or agents, incident to the performance of work under this contract, pursuant to the Standard Specifications for Road and Bridge Construction applicable to this contract.

In connection with the performance of work under this contract, the contractor agrees to comply with all applicable state and federal statutes relating to non-discrimination in employment. No otherwise qualified person shall be excluded from employment or otherwise be subject to discrimination in employment in any manner on the basis of age, race, religion, color, gender, national origin or ancestry, disability, arrest or conviction record (in keeping with s.111.32), sexual orientation, marital status, membership in the military reserve, honesty testing, genetic testing, and outside use of lawful products. This provision shall include, but not be limited to the following: employment, upgrading, demotion or transfer; recruitment or recruitment advertising; layoff or termination; rates of pay or other forms of compensation, and selection for training, including apprenticeship. The contractor further agrees to ensure equal opportunity in employment to all applicants and employees and to take affirmative action to attain a representative workforce.

The contractor agrees to post notices and posters setting forth the provisions of the nondiscrimination clause, in a conspicuous and easily accessible place, available for employees and applicants for employment.

If a state public official (section 19.42, Stats.) or an organization in which a state public official holds at least a 10% interest is a party to this agreement, this contract is voidable by the state unless appropriate disclosure is made to the State of Wisconsin Ethics Board.

Effective with August 2015 Letting

BID PREPARATION

Preparing the Proposal Schedule of Items

A General

- (1) Obtain bidding proposals as specified in **section 102** of the standard specifications prior to 11:45 AM of the last business day preceding the letting. Submit bidding proposals using one of the following methods:
 1. Electronic bid on the internet.
 2. Electronic bid on a printout with accompanying diskette or CD ROM.
 3. Paper bid under a waiver of the electronic submittal requirements.
- (2) Bids submitted on a printout with accompanying diskette or CD ROM or paper bids submitted under a waiver of the electronic submittal requirements govern over bids submitted on the internet.

- (3) The department will provide bidding information through the department's web site at:
<http://wisconsindot.gov/Pages/doing-bus/contractors/hcci/bid-let.aspx>

The contractor is responsible for reviewing this web site for general notices as well as information regarding proposals in each letting. The department will also post special notices of all addenda to each proposal through this web site no later than 4:00 P.M. local time on the Thursday before the letting. Check the department's web site after 5:00 P.M. local time on the Thursday before the letting to ensure all addenda have been accounted for before preparing the bid. When bidding using methods 1 and 2 above, check the Bid Express™ on-line bidding exchange at <http://www.bidx.com/> after 5:00 P.M. local time on the Thursday before the letting to ensure that the latest schedule of items Expedite file (*.ebs or *.00x) is used to submit the final bid.

- (4) Interested parties can subscribe to the Bid Express™ on-line bidding exchange by following the instructions provided at the www.bidx.com web site or by contacting:

Info Tech Inc.
5700 SW 34th Street, Suite 1235
Gainesville, FL 32608-5371
email: <mailto:customer.support@bidx.com>

- (5) The department will address equipment and process failures, if the bidder can demonstrate that those failures were beyond their control.
- (6) Contractors are responsible for checking on the issuance of addenda and for obtaining the addenda. Notice of issuance of addenda is posted on the department's web site at:
<http://wisconsindot.gov/Pages/doing-bus/contractors/hcci/bid-let.aspx>

or by calling the department at (608) 266-1631. Addenda can ONLY be obtained from the departments web site listed above or by picking up the addenda at the Bureau of Highway Construction, Room 601, 4802 Sheboygan Avenue, Madison, WI, during regular business hours.

- (7) Addenda posted after 5:00 PM on the Thursday before the letting will be emailed to the eligible bidders for that proposal. All eligible bidders shall acknowledge receipt of the addenda whether they are bidding on the proposal or not. Not acknowledging receipt may jeopardize the awarding of the project.

B Submitting Electronic Bids

B.1 On the Internet

- (1) Do the following before submitting the bid:
 1. Have a properly executed annual bid bond on file with the department.
 2. Have a digital ID on file with and enabled by Info Tech Inc. Using this digital ID will constitute the bidder's signature for proper execution of the bidding proposal.
- (2) In lieu of preparing, delivering, and submitting the proposal as specified in 102.6 and 102.9 of the standard specifications, submit the proposal on the internet as follows:
 1. Download the latest schedule of items reflecting all addenda from the Bid ExpressTM web site.
 2. Use ExpediteTM software to enter a unit price for every item in the schedule of items.
 3. Submit the bid according to the requirements of ExpediteTM software and the Bid ExpressTM web site. Do not submit a bid on a printout with accompanying diskette or CD ROM or a paper bid. If the bidder does submit a bid on a printout with accompanying diskette or a paper bid in addition to the internet submittal, the department will disregard the internet bid.
 4. Submit the bid before the hour and date the Notice to Contractors designates.
 5. Do not sign, notarize, and return the bidding proposal described in 102.2 of the standard specifications.
- (3) The department will not consider the bid accepted until the hour and date the Notice to Contractors designates.

B.2 On a Printout with Accompanying Diskette or CD ROM

- (1) Download the latest schedule of items from the Wisconsin pages of the Bid ExpressTM web site reflecting the latest addenda posted on the department's web site at:
<http://wisconsindot.gov/Pages/doing-bus/contractors/hcci/bid-let.aspx>

Use ExpediteTM software to prepare and print the schedule of items. Provide a valid amount for all price fields. Follow instructions and review the help screens provided on the Bid ExpressTM web site to assure that the schedule of items is prepared properly.

- (2) Staple an 8 1/2 by 11 inch printout of the ExpediteTM generated schedule of items to the other proposal documents submitted to the department as a part of the bidder's sealed bid. As a separate submittal not in the sealed bid envelop but due at the same time and place as the sealed bid, also provide the ExpediteTM generated schedule of items on a 3 1/2 inch computer diskette or CD ROM. Label each diskette or CD ROM with the bidder's name, the 4 character department-assigned bidder identification code from the top of the bidding proposal, and a list of the proposal numbers included on that diskette or CD ROM as indicated in the following example:

Bidder

Name

BN00

Proposals: 1, 12, 14, & 22

- (3) If bidding on more than one proposal in the letting, the bidder may include all proposals for that letting on one diskette or CD ROM. Include only submitted proposals with no incomplete or other files on the diskette or CD ROM.
- (4) The bidder-submitted printout of the ExpediteTM generated schedule of items is the governing contract document and must conform to the requirements of section 102 of the standard specifications. If a printout needs to be altered, cross out the printed information with ink or typewriter and enter the new information and initial it in ink. If there is a discrepancy between the printout and the diskette or CD ROM, the department will analyze the bid using the printout information.

- (5) In addition to the reasons specified in [section 102](#) of the standard specifications, proposals are irregular and the department may reject them for one or more of the following:
1. The check code printed on the bottom of the printout of the ExpediteTM generated schedule of items is not the same on each page.
 2. The check code printed on the printout of the ExpediteTM generated schedule of items is not the same as the check code for that proposal provided on the diskette or CD ROM.
 3. The diskette or CD ROM is not submitted at the time and place the department designates.

C Waiver of Electronic Submittal

- (1) The bidder may request a waiver of the electronic submittal requirements. Submit a written request for a waiver in lieu of bids submitted on the internet or on a printout with accompanying diskette or CD ROM. Use the waiver that was included with the paper bid document sent to the bidder or type up a waiver on the bidder's letterhead. The department will waive the electronic submittal requirements for a bidding entity (individual, partnership, joint venture, corporation, or limited liability company) for up to 4 individual proposals in a calendar year. The department may allow additional waivers for equipment malfunctions.
- (2) Submit a schedule of items on paper conforming to [section 102](#) of the standard specifications. The department charges the bidder a \$75 administrative fee per proposal, payable at the time and place the department designates for receiving bids, to cover the costs of data entry. The department will accept a check or money order payable to: "Wisconsin, Dept. of Transportation."
- (3) In addition to the reasons specified in [section 102](#) of the standard specifications, proposals are irregular and the department may reject them for one or more of the following:
 1. The bidder fails to provide the written request for waiver of the electronic submittal requirements.
 2. The bidder fails to pay the \$75 administrative fee before the time the department designates for the opening of bids unless the bidder requests on the waiver that they be billed for the \$75.
 3. The bidder exceeds 4 waivers of electronic submittal requirements within a calendar year.
- (4) In addition to the reasons specified in [section 102](#) of the standard specifications, the department may refuse to issue bidding proposals for future contracts to a bidding entity that owes the department administrative fees for a waiver of electronic submittal requirements.

PROPOSAL BID BOND

DT1303 1/2006

Wisconsin Department of Transportation

Proposal Number	Project Number	Letting Date
Name of Principal		
Name of Surety	State in Which Surety is Organized	

We, the above-named Principal and the above-named Surety, are held and firmly bound unto the State of Wisconsin in the sum equal to the Proposal Guaranty for the total bid submitted for the payment to be made; we jointly and severally bind ourselves, our heirs, executors, administrators, successors and assigns. The condition of this obligation is that the Principal has submitted a bid proposal to the State of Wisconsin acting through the Department of Transportation for the improvement designated by the Proposal Number and Letting Date indicated above.

If the Principal is awarded the contract and, within the time and manner required by law after the prescribed forms are presented for signature, enters into a written contract in accordance with the bid, and files the bond with the Department of Transportation to guarantee faithful performance and payment for labor and materials, as required by law, or if the Department of Transportation shall reject all bids for the work described, then this obligation shall be null and void; otherwise, it shall be and remain in full force and effect. In the event of failure of the Principal to enter into the contract or give the specified bond, the Principal shall pay to the Department of Transportation **within 10 business days of demand** a total equal to the Proposal Guaranty as liquidated damages; the liability of the Surety continues for the full amount of the obligation as stated until the obligation is paid in full.

The Surety, for value received, agrees that the obligations of it and its bond shall not be impaired or affected by any extension of time within which the Department of Transportation may accept the bid; and the Surety does waive notice of any such extension.

IN WITNESS, the Principal and Surety have agreed and have signed by their proper officers and have caused their corporate seals to be affixed this date: **(DATE MUST BE ENTERED)**

PRINCIPAL

(Company Name) **(Affix Corporate Seal)**

(Signature and Title)

(Company Name)

(Signature and Title)

(Company Name)

(Signature and Title)

(Company Name)

(Signature and Title)

NOTARY FOR PRINCIPAL

(Date)

State of Wisconsin)
) ss.
_____ County)

On the above date, this instrument was acknowledged before me by the named person(s).

(Signature, Notary Public, State of Wisconsin)

(Print or Type Name, Notary Public, State of Wisconsin)

(Date Commission Expires)

Notary Seal

(Name of Surety) **(Affix Seal)**

(Signature of Attorney-in-Fact)

NOTARY FOR SURETY

(Date)

State of Wisconsin)
) ss.
_____ County)

On the above date, this instrument was acknowledged before me by the named person(s).

(Signature, Notary Public, State of Wisconsin)

(Print or Type Name, Notary Public, State of Wisconsin)

(Date Commission Expires)

Notary Seal

IMPORTANT: A certified copy of Power of Attorney of the signatory agent must be attached to the bid bond.

CERTIFICATE OF ANNUAL BID BOND

DT1305 8/2003

Wisconsin Department of Transportation

Time Period Valid (From/To)	
Name of Surety	
Name of Contractor	
Certificate Holder	Wisconsin Department of Transportation

This is to certify that an annual bid bond issued by the above-named Surety is currently on file with the Wisconsin Department of Transportation.

This certificate is issued as a matter of information and conveys no rights upon the certificate holder and does not amend, extend or alter the coverage of the annual bid bond.

Cancellation: Should the above policy be cancelled before the expiration date, the issuing surety will give thirty (30) days written notice to the certificate holder indicated above.

(Signature of Authorized Contractor Representative)

(Date)

March 2010

LIST OF SUBCONTRACTORS

Section 66.0901(7), Wisconsin Statutes, provides that as a part of the proposal, the bidder also shall submit a list of the subcontractors the bidder proposes to contract with and the class of work to be performed by each. In order to qualify for inclusion in the bidder's list a subcontractor shall first submit a bid in writing, to the general contractor at least 48 hours prior to the time of the bid closing. The list may not be added to or altered without the written consent of the municipality. A proposal of a bidder is not invalid if any subcontractor and the class of work to be performed by the subcontractor has been omitted from a proposal; the omission shall be considered inadvertent or the bidder will perform the work personally.

No subcontract, whether listed herein or later proposed, may be entered into without the written consent of the Engineer as provided in Subsection 108.1 of the Standard Specifications.

[illegible]

DECEMBER 2000

**CERTIFICATION REGARDING DEBARMENT, SUSPENSION, AND OTHER
RESPONSIBILITY MATTERS - PRIMARY COVERED TRANSACTIONS**

Instructions for Certification

1. By signing and submitting this proposal, the prospective contractor is providing the certification set out below.
2. The inability of a person to provide the certification required below will not necessarily result in denial of participation in this covered transaction. The prospective contractor shall submit an explanation of why it cannot provide the certification set out below. The certification or explanation will be considered in connection with the department or agency's determination whether to enter into this transaction. However, failure of the prospective contractor to furnish a certification or an explanation shall disqualify such person from participation in this transaction.
3. The certification in this clause is a material representation of fact upon which reliance was placed when the department determined to enter into this transaction. If it is later determined that the contractor knowingly rendered an erroneous certification in addition to other remedies available to the Federal Government the department may terminate this transaction for cause or default.
4. The prospective contractor shall provide immediate written notice to the department to whom this proposal is submitted if at any time the prospective contractor learns that its certification was erroneous when submitted or has become erroneous by reason of changed circumstances.
5. The terms "covered transaction," "debarred," "suspended," "ineligible," "lower tier covered transaction," "participant," "person," "primary covered transaction," "principal," "proposal," and "voluntarily excluded," as used in this clause, have the meanings set out in the Definitions and Coverage sections of the rules implementing Executive Order 12549. You may contact the department to which this proposal is being submitted for assistance in obtaining a copy of those regulations.
6. The prospective contractor agrees by submitting this proposal that, should this contract be entered into, it shall not knowingly enter into any lower tier covered transaction with a person who is debarred, suspended, declared ineligible, or voluntarily excluded from participation in this covered transaction, unless authorized by the department entering into this transaction.
7. The prospective contractor further agrees by submitting this proposal that it will include the clause titled "Certification Regarding Debarment, Suspension, Ineligibility and Voluntary Exclusion-Lower Tier Covered Transaction," which is included as an addendum to PR-1273 - "Required Contract Provisions Federal Aid Construction Contracts," without

modification, in all lower tier covered transactions and in all solicitations for lower tier covered transactions.

8. The contractor may rely upon a certification of a prospective subcontractor/materials supplier that it is not debarred, suspended, ineligible, or voluntarily excluded from the covered transaction, unless it knows that the certification is erroneous. A contractor may decide the method and frequency by which it determines the eligibility of its principals. Each contractor may, but is not required to, check the Disapproval List (telephone # 608/266/1631).
9. Nothing contained in the foregoing shall be construed to require establishment of a system of records in order to render in good faith the certification required by this clause. The knowledge and information of a contractor is not required to exceed that which is normally possessed by a prudent person in the ordinary course of business dealings.
10. Except for transactions authorized under paragraph 6 of these instructions, if a contractor in a covered transaction knowingly enters into a lower tier covered transaction with a person who is suspended, debarred, ineligible or voluntarily excluded from participation in this transaction, in addition to other remedies available to the Federal Government, the department may terminate this transaction for cause or default.

Certification Regarding Debarment, Suspension, and Other Responsibility Matters - Primary Covered Transactions

- (1) The prospective contractor certifies to the best of its knowledge and belief, that it and its principals:
 - (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency;
 - (b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements or receiving stolen property;
 - (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offense enumerated in paragraph (1)(b) of this certification; and
 - (d) Have not within a three-year period preceding this proposal had one or more public transactions (Federal, State or local) terminated for cause or default.
- (2) Where the prospective contractor is unable to certify to any of the statements in this certification, such prospective contractor shall attach an explanation to this proposal.

Special Provisions

Table of Contents

Article	Description	Page #
1.	General.....	2
2.	Scope of Work.	2
3.	Prosecution and Progress.	2
4.	Traffic.	3
5.	Holiday Work Restrictions.	4
6.	Utilities.....	4
7.	Railroad Insurance and Coordination. – Wisconsin Central Ltd.	4
8.	Railroad Insurance and Coordination. – Union Pacific Railway.....	6
9.	Notice to Contractor, No Field Office.	7
10.	Removing Signs Type II, Item 638.2602.....	7
11.	Traffic Control.	8
12.	Marking Line Paint 4-Inch, Item 646.1005.....	9
13.	Locating No-Passing Zones, Item 648.0100.....	9
14.	Chip Seal, SPV.0180.001.....	9
15.	Scrub Seal, Item SPV.0180.002.....	13
16.	Fog Seal, SPV.0180.003.	18

SPECIAL PROVISIONS

1. General.

Perform the work under this construction contract for Project 1000-18-06, SFY 2018 LIL Chip Seals – East, Northwest Region, Various Highways, Barron, Chippewa, Clark, Dunn, Eau Claire, and Rusk Counties, Wisconsin as the plans show and execute the work as specified in the State of Wisconsin, Department of Transportation, Standard Specifications for Highway and Structure Construction, 2018 Edition, as published by the department, and these special provisions.

If all or a portion of the plans and special provisions are developed in the SI metric system and the schedule of prices is developed in the US standard measure system, the department will pay for the work as bid in the US standard system.

100-005 (20170615)

2. Scope of Work.

The work under this contract shall consist of chip sealing, scrub sealing, fog sealing, signing, pavement marking, and traffic control and all incidental items necessary to complete the work as shown on the plans and included in the proposal and contract.

104-005 (20090901)

3. Prosecution and Progress.

Begin work within ten calendar days after the engineer issues a written notice to do so.

Provide the start date to the engineer in writing within a month after executing the contract but at least 14 calendar days before the preconstruction conference. Upon approval, the engineer will issue the notice to proceed within ten calendar days before the approved start date.

To revise the start date, submit a written request to the engineer at least two weeks before the intended start date. The engineer will approve or deny that request based on the conditions cited in the request and its effect on the department's scheduled resources.

Lane Closure Restrictions

Between Memorial Day and Labor Day no work on Friday from noon to 6:00 PM and on Sunday from noon to 6:00 PM for the following segment:

- USH 8, Cameron – Ladysmith, (S. Junction CTH SS to Chippewa River), Barron and Rusk County

Work Restrictions

Do not begin construction operations until after July 13, 2018 (due to the Wisconsin Farm Technology Days event) for the following segment:

- STH 73, Neillsville – Thorp (Hunt Street to STH 29 East), Clark County

4. Traffic.

Coordinate the single-lane flagging operations in a manner which cause as little delay to the traveling public as possible.

The maximum length between flaggers cannot exceed a 1/2 mile for work from Monday through Friday before 9:00 AM and between 4:00 PM and 7:00 PM for the following segment:

- USH 12, Altoona – Fall Creek (Industrial Drive to Fall Creek Creek), Eau Claire County

Except as otherwise authorized by engineer, the maximum length of the single-lane work zone shall be no more than 1 mile.

Do not park or store equipment, vehicles, or construction materials within 30 feet of the edge of the traffic lane of any roadway during non-working hours.

Wisconsin Lane Closure System Advance Notification

Provide the following advance notification to the engineer for incorporation into the Wisconsin Lane Closure System (LCS).

TABLE 108-1 CLOSURE TYPE AND REQUIRED MINIMUM ADVANCE NOTIFICATION

Closure type with height, weight, or width restrictions (available width, all lanes in one direction <16')	MINIMUM NOTIFICATION
Lane and shoulder closures	7 calendar days
Full roadway closures	7 calendar days
Ramp closures	7 calendar days
Detours	7 calendar days
Closure type without height, weight, or width restrictions (available width, all lanes in one direction ≥16')	MINIMUM NOTIFICATION
Lane and shoulder closures	3 business days
Ramp closures	3 business days
Modifying all closure types	3 business days

Discuss LCS completion dates and provide changes in the schedule to the engineer at weekly project meetings in order to manage closures nearing their completion date.

Stp-108-057 (20161130)

5. Holiday Work Restrictions.

Do not perform work on, nor haul materials of any kind along or across any portion of the highway carrying traffic, and entirely clear the traveled way and shoulders of such portions of the highway of equipment, barricades, signs, lights, and any other material that might impede the free flow of traffic during the following holiday periods:

- From Noon Friday, May 25, 2018 until 6:00 AM, Tuesday, May 29, 2018 for Memorial Day;
- From Noon Tuesday, July 3, 2018 until 6:00 AM, Thursday, July 5, 2018 for Independence Day.

stp-107-005 (20050502)

6. Utilities.

This contract does not come under the provision of Administrative Rule Trans 220.
stp-107-065 (20080501)

Underground and overhead utility facilities are located within the project limits. There are no utility conflicts anticipated for this project.

Coordinate construction activities with a call to Diggers hotline or a direct call to the underground facilities in the area, as required per state statutes. Use caution to maintain the integrity of utilities. Coordinate with the engineer to adjust plans as needed to avoid any unanticipated utility conflicts.

7. Railroad Insurance and Coordination. – Wisconsin Central Ltd.

A Description

Comply with standard spec 107.17 for all work affecting Wisconsin Central Ltd (CN) property and any existing tracks.

A.1 Railroad Insurance Requirements

In addition to standard spec 107.26, provide railroad protective liability insurance coverage as specified in standard spec 107.17.3. Insurance is filed in the name of Wisconsin Central Ltd and Its Parents (CN).

Notify evidence of the required coverage, and duration to Jackie Macewicz, CN Manager Public Works, 1625 Depot Street, Stevens Point, WI 54481; Telephone (715) 345-2503; E-mail: Jackie.macewicz@cn.ca also send a copy to the following: Anna Davey, NW Region Railroad Coordinator, 1701 N 4th Street, Superior, WI, 54880, Telephone (715) 392-7960, E-mail: anna.davey@dot.wi.gov

Include the following information on the insurance document:

Project: 1000-18-06

Project Location: Barron and Rusk County, WI

Route Name: USH 8

Railroad Subdivision Barron Sub

Crossing ID: 691201W - 691252G (Project parallel to WCL Barron Sub on USH 8)

Railroad Milepost: 96.54 - 121.3

Work Performed: Chip and Fog Seal of USH 8

A.2 Train Operation

Approximately two (2) through freight trains operate daily at up to 40 mph.

A.3 Names and Addresses of Railroad Representatives for Consultation and Coordination

Construction Contact

Jackie Macewicz, CN Manager Public Works, 1625 Depot St., Stevens Point, WI, 54481; Telephone (715) 345-2503; E-mail jackie.macewicz@cn.ca for consultation on railroad requirements during construction.

Amend standard spec 108.4 to include the railroad in the distribution of the initial bar chart, and monthly schedule updates. The bar chart shall specifically show work involving coordination with the railroad.

Flagging Contact

Submit by US Mail a "Request for Flagging Services and Cable Location" form with prepayment to: Mary Ellen Carmody, CN, 24002 Vreeland Road, Flat Rock, MI, 48134. The form can be obtained at:

<http://www.cn.ca/en/delivering-responsibly/safety/erailsafe/utility-installations>.

Requests for flagging and cable locates can take up to five business days after the railroad receives the paperwork. Reference the Wisconsin Milepost and Subdivision located in A.1. Advise Ms. Carmody that the flagging services are to be billed at the rate for a public highway project.

Cable Locate Contact

In addition to contacting Diggers Hotline, follow the procedure listed under Flagging Contact.

Wisconsin Central Ltd (CN) will only locate railroad owned facilities buried in the railroad right-of-way. The railroad does not locate any other utilities.

A.4 Work by Railroad

The railroad will perform the work described in this section, except for work described in other special provisions and will be accomplished without cost to the contractor. None

A.5 Temporary Grade Crossing

If a temporary grade crossing is desired, submit a written request to the railroad representative named in A.2 at least 40 days prior to the time needed. Approval is subject to the discretion of the railroad. The department has made no arrangements for a temporary grade crossing.

8. Railroad Insurance and Coordination. – Union Pacific Railway.

A Description

Comply with standard spec 107.17 for all work affecting Union Pacific Railway Company property and any existing tracks.

A.1 Railroad Insurance Requirements

In addition to standard spec 107.26, provide railroad protective liability insurance coverage as specified in standard spec 107.17.3. Insurance is filed in the name of Union Pacific Railway Company.

Notify evidence of the required coverage, and duration to Danielle Allen, UP Contract Administrator, 1400 Dodge Street, Stop 1690, Omaha, NE 68179; Telephone (402) 544-8020; E-mail: danielleallen@up.com also send a copy to the following: Anna Davey, NW Region Railroad Coordinator, 1701 N 4th Street, Superior, WI, 54880, Telephone (715) 392-7960, E-mail: anna.davey@dot.wi.gov

Include the following information on the insurance document:

Project: 1000-18-06
Project Location: Elk Mound, WI
Route Name: USH 12, Dunn County, WI
Railroad Subdivision Altoona Sub
Crossing ID: 183924J – 183933H
Railroad Milepost: 76.52 – 80.3
Work Performed: Chip and Fog Seal on USH 12

A.2 Train Operation

Approximately four through freight trains operate daily at up to 50 mph. There are no switching movements at this location.

A.3 Names and Addresses of Railroad Representatives for Consultation and Coordination

Construction Contact

John Venice, Manager Special Projects – Industry & Public Projects Engineering Department, 101 North Wacker Drive – Suite 1920, Chicago, IL 60606; Telephone (312) 777-2043; E-mail jnvenice@up.com, for consultation on railroad requirements during construction.

Amend standard spec 108.4 to include the railroad in the distribution of the initial bar chart, and monthly schedule updates. The bar chart shall specifically show work involving coordination with the railroad.

Flagging Contact

See Construction Contact. If more than thirty (30) days of flagging is required contact UP 30 days prior to needing a flagger on site. Reference the Wisconsin Milepost and Subdivision located in A.1.

Cable Locate Contact

In addition to contacting Diggers Hotline, contact the UP Call Before You Dig line at (800) 336-9193 at least five working days before the locate is needed. Normal business hours are 6:30 AM to 6:30 PM, Central Time, Monday through Friday, except holidays and are subject to change. Calls will be routed at all times in case of an emergency. Reference the Wisconsin Milepost and Subdivision located in A.1.

UP will only locate railroad owned cable buried in the railroad right-of-way. The railroad does not locate any other utilities.

A.4 Work by Railroad

The railroad will perform the work described in this section, except for work described in other special provisions and will be accomplished without cost to the contractor. None

A.5 Temporary Grade Crossing

If a temporary grade crossing is desired, submit a written request to the railroad representative named in A.2 at least 40 days prior to the time needed. Approval is subject to the discretion of the railroad. The department has made no arrangements for a temporary grade crossing.

9. Notice to Contractor, No Field Office.

Due to the excessive travel distance between the various project locations, a central field office will not be used for the project.

10. Removing Signs Type II, Item 638.2602.

This work shall be according to the pertinent requirements of standard spec 638 and as provided here.

Type II signs are the department's property. All department signs removed, and not identified for reuse, shall be separated, plywood from aluminum signs, and the aluminum signs shall be palletized for shipment and handling with a forklift. Contact DTSD Sign Shop Coordinator Steve Allard at (715) 855-7671 at least three business days prior to delivery to coordinate shipment to be delivered to the DTSD Sign Shop Distribution Center at one of the following locations:

Dunn County Highway Shop, 3303 USH 12 East, Menomonie, WI 54751

LaCrosse County Highway Shop, 301 Carlson Rd, West Salem, WI 54669

Price County Highway Shop, 704 N. Lake Ave, Phillips, WI 54555

Washburn County Highway Shop, 1600 CTH H, Spooner, WI 54801

Wood County Highway Shop, 555 17th Ave North, Wisconsin Rapids, WI 54495

11. Traffic Control.

Supplement standard spec 643.3.1 with the following.

Lane closures and flagging operations shall only be allowed during daytime hours. Do not implement a lane closure during inclement weather or poor visibility.

When traffic control devices are not in use, they shall be covered and/or removed from the traveled way and shoulders of the roadway.

All signs including “Loose Gravel” signs, “Road Work Next XX Miles” signs, “No Center Stripe” signs, “35 mph” advisory signs, “Do Not Pass” signs, “Pass With Care” signs, and all sign supports required shall be incidental to the item of Traffic Control.

Provide to the engineer, County Sheriff’s Department, and the State Patrol District Headquarters responsible for that county with the current telephone number(s) which the contractor or their representative can be contacted at all times in the event a safety hazard develops.

Keep appropriate emergency officials informed of routes to provide emergency services. Utilize two-way radios, and an additional flag person or persons, within lane closure areas, in order to positively direct, control, and safeguard traffic through the work zone.

All contractor vehicles or equipment operating within the project limits shall be equipped with and have flashing yellow lights operating.

The third flag person is required to be located at the area of the moving operation to safely guide traffic around the equipment and personnel working at the moving operation.

A flagger shall be at all side roads within the work zone.

All department owned signs that are removed by the contractor because of interference with construction operations shall, unless otherwise authorized by the engineer, be promptly replaced as directed by the engineer. At no time may stop signs be removed or moved without flag persons present.

Use signs to designate the no-passing zones and to warn traffic during the time when no-passing zone and centerline pavement markings are not in place on intermediate or final surfaces open to through traffic. The department will allow the use of such signs in lieu of no-passing zone and centerline pavement markings for up to three calendar days following completion of each pavement layer. Such signs shall be in place prior to the time when the pre-existing pavement marking is obliterated and shall be removed following placement of the no-passing zone and centerline pavement markings. Signs may be placed on portable supports unless the sign will be used continuously at the same location for seven or more days. The following signs shall be used:

- (1) “Do Not Pass” (R4-1, 24” x 30”) and the existing “No Passing Zone” pennant (W14-3) at the beginning of each no-passing zone.
- (2) Additional “Do Not Pass” (R4-1, 24” x 30”) signs within any no-passing zone that continues beyond an intersection with a state or county trunk highway or that exceeds one mile in length. Place an additional sign where traffic enters a no- passing zone from such an intersection and wherever necessary to provide a maximum one-mile sign spacing within any single zone.
- (3) “Pass With Care” (R4-2, 24” x 30”) at the end of each no-passing zone.
- (4) “No Center Stripe” (W8-12, 48” x 48” minimum) at the beginning of the unmarked area, at two-mile intervals throughout the unmarked area, and at locations where traffic enters the unmarked area from intersections with state trunk and county trunk highways.

12. Marking Line Paint 4-Inch, Item 646.1005.

Add the following to standard spec 646.3.1.1:

Apply permanent pavement markings to the upper layer of pavements open to through traffic within three calendar days after completing the fog sealing operation. In the event of adverse weather or other constraints to the marking operations, resume with the approval of the engineer when conditions permit.

13. Locating No-Passing Zones, Item 648.0100.

For this project, the spotting sight distance in areas with a 55 mph posted speed limit is 0.21 miles (1108 feet).
stp-648-005 (20060512)

14. Chip Seal, SPV.0180.001.

A Description

Construct a chip seal surface treatment as described in standard spec 475 and as hereinafter provided.

B Materials

B.1 Asphaltic Material

Provide CRS-2P or HFRS-2P, Asphaltic material for chip seal. The CRS-2P and HFRS-2P, asphaltic material for chip seal shall meet the following requirements for the type and grade specified. Only Asphaltic Material supplied from a certified source is approved for use.

Emulsified asphalt compatibility with aggregate shall be verified prior to construction. This shall be done through documentation from material provider.

Emulsified Asphalt

The emulsified asphalt shall meet the requirements of AASHTO M-316 subject to the following modification:

- Polymer-modified cationic emulsified asphalt, (CRS-2P), and polymer-modified anionic emulsified asphalt, (HFRS-2P) shall be produced by using polymer modified base asphalt only. The use of Latex modification shall not be allowed. Any emulsion not meeting this requirement shall not be used.

B.2 Seal Coat

Provide aggregate conforming to standard spec 460. Use aggregates, uniform in quality and free from wood, bark, roots, and other deleterious materials. Gradation and are specified in Table 1.

Table 1 (Values are the percent passing the sieve)

Sieve Size	Percent Passing By Weight #1	Percent Passing By Weight #2	Percent Passing By Weight #3
3/8 inch (9.5mm)	100	100	100
#4 (4.75mm)	90-100	90-100	85-100
#8 (2.36mm)	45-75	5-10	10-40
#16 (1.19mm)			0-10
#40 (425mm)	0-8	0	
#200 (75mm)	0-2		0-1

B.3 Water

Use potable water, compatible with the seal coat.

B.4 Mix Design

If the aggregate material specified in Table 1 is unavailable or is cost prohibitive to obtain, the contractor may submit a proposed aggregate and emulsion design. The design shall include a listing of recent projects and roadways where the design has been utilized within the last four years. The proposed alternative design must be reviewed and approved by the engineer in writing prior to using on the project.

C Construction

C.1 General

Construct chip seal to the width as shown in the plans or as directed by the engineer. Use asphaltic material of the type and grade the special provisions designate.

C.2 Equipment

Distributor: Use a distributor as specified in standard spec 455.3.2.2.

Aggregate Spreader: Use a self-propelled mechanical type aggregate spreader capable of distributing the aggregate uniformly to the required width and at the designed rate. Use a self-propelled type mounted on pneumatic-tired wheels.

Rollers: Provide a minimum of two self-propelled rollers. At least one roller will be a pneumatic-tire roller. Steel-wheel rollers must weigh between 6 and 9 tons.

Brooms: Provide motorized brooms with a positive means of controlling vertical pressure and capable of cleaning the road surface prior to spraying asphaltic material and removing loose aggregate after chip seal.

C.3 Weather Limitations

Construct chip seal operations (including traffic restrictions on the freshly constructed chip seal) according to the following:

- Not before May 1 or after August 31.
- Work only during daylight hours.
- Start when the pavement and air temperature are 60°F and rising.
- The road surface is dry and there shall be no standing water.
- Do not apply before impending rains if rain might damage the material before placing and rolling the cover aggregates.

C.4 Road Surface Preparation

Immediately before applying the asphaltic material clean the existing surface with a power broom or other suitable equipment to remove dirt, clay or other objectionable matter. Clean depressions not reached by the power broom, using hand brooming.

Remove vegetation from cracks or joints.

Cover iron (manholes, gate valve covers, catch basins, sensors, etc.) to prevent adherence of the asphaltic material. Suitable covering includes plywood disks, sand, Kraft paper, roofing felt or other approved methods. Remove the protective coverings before opening the road to traffic.

C.5 Application of Asphaltic Material

Begin the rate of application for the asphaltic material at a rate of 0.36 gallons per square yard or as determined and agreed upon by the engineer. Inspect the aggregate in the wheel paths for proper embedment. Make adjustments to the rate of application, if necessary. Construct one full lane width at a time. Cover previously applied material as required to

create transverse joints without overlapping. Make additional adjustments to the rate of application during the project if needed.

Limit the length of any spread to that which can be covered immediately with aggregate and rolled within 30 minutes. Limit the distance between the bitumen distributor and the aggregate spreader to 150 feet.

C.6 Application of Aggregate

Apply the aggregates at a rate of 18 pounds per square yard or as determined and agreed upon by the engineer.

Immediately after the asphaltic material has been sprayed, apply the aggregates. The speed of the spreader shall be such that the aggregates are not turned over, and starting and stopping of the spreader is minimized. The edges of the aggregate applications shall be sharply defined. Previously used (sweeping) aggregates will not be allowed.

C.7 Rolling Operations

Complete the initial rolling immediately after applying the aggregate. Proceed at a recommended speed less than or equal to 5 miles per hour, to prevent turning over aggregate.

Begin at the edges and continue to the center, lapping 1/2 the roller width on each successive pass. After this initial rolling, perform subsequent rolling using a combination of steel-wheel rollers and pneumatic tire rollers or pneumatic tire rollers only until thoroughly embedding the aggregate and the surface is smooth and uniform in texture.

The total compacting width of each roller shall exceed 5 feet.

C.8 Sweeping

Within 24 hours of the chip seal construction, sweep off the surplus aggregate on all chip seal operations including paved shoulders, asphaltic curb locations, intersecting highways, and bridges. Sweep with care to not dislodge aggregate that is setting. Re-sweep areas after the initial sweeping. Dispose of the surplus chip seal aggregate in a manner satisfactory to the engineer.

C.9 Protection of the Surface

No traffic is permitted on the chip seal road surface until after the specified rolling has been completed and the asphaltic material has set and will not pick up on vehicle tires.

D Measurement

The department will measure Chip Seal by the square yard, acceptably completed.

E Payment

The department will pay for measured quantities at the unit price under the following work items:

ITEM NUMBER	DESCRIPTION	UNIT
SPV.0180.001	Chip Seal	SY

Payment is full compensation for preparing the surface; for furnishing, heating, and applying asphaltic material; for furnishing, drying or moistening, applying, and rolling the cover aggregate; for brooming, finishing, and maintaining the surface.

15. Scrub Seal, Item SPV.0180.002.

A Description

Construct a scrub seal surface treatment as described in standard spec 475 and as hereinafter provided.

B Materials

B.1 Polymer Modified Bituminous Rejuvenating Emulsion Binder – Grade CMS-2P

Emulsion Property	MIN	MAX	Test Method
Viscosity @ 50°C (122°F) (cPs)	110	880	ASTM D7226
Residue by Evaporation, W%, MIN	65		ASTM D6943 ¹
Sieve, W%, MAX ²		0.1	AASHTO T59
Particle Charge	POSITIVE		AASHTO T59
Demulsibility 35 mL, 0.8% Sodium Dioctyl sulfosuccinate%		20.0	AASHTO T59
Storage Stability, W%, 24 Hours ²		1.0	AASHTO T59
Residue Property (By Vacuum Distillation)			AASHTO D7403
G*/SINδ @ 52°C , (kPa)	2.2		AASHTO T315
MSCR @ 10°C, %R @ 3.2kPa, %	55		ASTM D7405
Penetration @ 4°C, MIN ³	40		AASHTO T49
Rejuvenating Agent Property			
Viscosity, 60°C (140°F), cPs	50	300	ASTM D4402
Flash Point, COC, F	>425		ASTM D92
Saturates, W%		30	ASTM D2007
Solubility in N-Pentane, W%	99.0		ASTM D2007
Mass Loss After RTFO or TFO, W%		6.5	AASHTO T240 OR T179
Viscosity Ratio		3.0	AASHTO T179

1. ASTM D7404 is an acceptable alternate test method for determination of % residue in emulsion.
2. This test requirement on representative samples is waived if successful application of the material has been achieved in the field.
3. Test conditions: 200g at 60s

B.2 Coarse Aggregate

The coarse aggregate shall be 100% crushed material from quarried stone, natural gravel or other high quality aggregate excluding sources containing magnesium or calcium carbonite and meet the following requirements:

B.4 Physical Requirements

Description	MIN	MAX	Test Method
L.A. Abrasion Loss @ 500 Revolutions, %		35	AASHTO T96
Deleterious Material, W%		1.0	AASHTO T113
Crushed Pieces, 1 Fractured Face, %	100		WISDOT CMM 8-60
Sodium Sulfate Soundness Test, 5 Cycle, R4,%	15		AASHTO T104

B.5 Grading Requirements – ASTM C-117

Sieve		Type I
1 inch	(25 mm)	100
3/4 inch	(19 mm)	100
1/2 inch	(12.5 mm)	100
3/8 inch	(9.5 mm)	100
5/16 inch	(8.0 mm)	90-100
No. 4	(4.75 mm)	10-80
No. 8	(2.36 mm)	5-30
No. 16	(1.18 mm)	0-15
No. 200	(75 um)	0-5

C Construction

C.1 Equipment

All equipment required for performance of the work shall be approved before construction is to begin, and shall be maintained in satisfactory operating condition. The contractor shall furnish an accurate thermometer, hand brooms and other small tools and equipment essential for the completion of the work.

Pressure Distributor

The pressure distributor shall have a computerized rate control that automatically adjusts the distributor's pump to the ground speed. The pressure distributor shall be capable of heating and re-circulating the bituminous binder to the specified temperature. The proper nozzles shall be used for the material and rate specified.

Scrub Broom

The scrub broom frame shall be constructed such that the scrub broom is attached to the distributor truck. The scrub broom must be equipped with the means to mechanically raise and lower the scrub broom off and onto the road surface at designated points of completion and start up. It shall be towable in the elevated position. The weight of the broom assembly shall be such that it does not squeegee the emulsion off the roadway surface.

The main body of the scrub broom shall be a frame minimum 6'-9" wide and 10 feet long. The maximum transverse rigid frame width at any point shall not exceed 6'-9". The nearest and furthest members, paralleling the back of the spreader truck, and diagonal members shall be equipped with emulsion scrub brooms. The leading member and the trailing member shall have broom heads angled at 10 to 15 degrees off the centerline of the supporting member. Each individual emulsion scrub broom attached to the scrub broom assembly shall be 3 1/2" w x 6 1/2" h x 16" l and have stiff nylon bristles. Bristle height is to be maintained at a minimum of 5 inches. The scrub broom shall be equipped with hinged wing assemblies attached to the main body not to exceed 4'-6" per side, with diagonals and equipped with emulsion scrub brooms. The purpose of the maximum rigid frame width and the hinged wing extensions is not only for maximum width of 16 feet but to maintain the scrubbing process consistently as surface textures and conditions along with cross-sections change along the existing roadway surface.

NOTE: The contractor must supply a scrub broom as described for the purpose of scrubbing the polymer modified asphaltic rejuvenating emulsion. If the contractor fails to supply the scrub broom specified, the project will be shut down.

Aggregate Spreader

The aggregate spreader shall be self-propelled and shall be equipped with hoppers, revolving cylinders and adjustments necessary to produce a uniform distribution of material at the specified rate.

Pneumatic Tire Roller

The pneumatic tire rollers shall weigh a minimum of five tons.

C.2 Pre-Paving On-Site Meeting

A meeting between the contractor and engineer will be held at the project site prior to beginning work. The agenda for this meeting will include:

- Review of contractor's detailed work schedule
- Review of the traffic control plan
- Inspection of equipment
- Calibration and adjustment to equipment as needed

C.3 Weather Limitations

The stress absorbing membrane interlayer scrub seal shall be placed when the pavement and atmospheric temperature is 50°F and rising. Placement is not permitted if it is raining, when the pavement surface has standing water, or when temperatures are forecasted to be below 40°F within 24 hours of placement.

C.4 Qualifications

The contractor shall have had a minimum of five (5) years' experience in the application of polymer modified asphaltic emulsion and have successfully completed at least three similar type projects utilizing the scrub-broom. References shall be supplied upon request.

C.5 General

The contractor shall follow the construction methods as described:

- Preparation of the surface shall include removal of all vegetation, dirt and debris from the roadway. The surface shall be cleaned by the contractor and shall be reasonably dry when the bituminous binder is applied. Material cleaned from the surface shall be properly disposed of.
- Construct the scrub seal to the width as shown in the plans or as directed by the engineer. The edges of the limits of the scrub seal application on both sides of the road shall be maintained in a neat and uniform line.
- Immediately following application of the polymer modified bituminous rejuvenating emulsion binder to the roadway surface, the material shall be scrubbed with the scrub broom for the purpose of forcing the emulsion into the existing surface voids and distributing the emulsion over variable roadway surface textures and conditions.
- The application of the polymer modified asphaltic rejuvenating emulsion and scrub broom operation shall cease +/- 40 feet prior to the end of the road section or intersection. The remaining polymer modified asphaltic rejuvenating emulsion shall be drug out by the scrub broom, and the remaining emulsified material required to complete the pass shall be applied only by the distributor at the specified rate.
- The specified aggregate shall be spread uniformly onto the bituminous binder prior to the emulsion breaking and as agreed to by the engineer.
- Projects with segments greater than 20,000 square yards shall use a minimum of three rollers. Rollers shall proceed at a maximum speed of 5 mph. The entire surface shall receive a minimum of two roller passes. The first roller pass shall be performed within one minute of aggregate spreading.
- Sweeping of the completed surface shall be accomplished prior to unrestricted use by traffic. The entire surface shall be clean of all loose material within 24 hours and prior to placement of any surface course.
- Cover iron (manholes, gate valve covers, catch basins, sensors, etc.) to prevent adherence of the asphaltic material. Suitable covering includes plywood disks, sand, Kraft paper, roofing felt or other approved methods. Remove the protective coverings before opening the road to traffic.

C.6 Application of Bituminous Binder and Coarse Aggregate

The bituminous binder shall be heated to specified temperature and uniformly placed to prevent ridges or streaks in the surface.

C.7 Bituminous Binder

The bituminous binder shall be applied at a temperature of 110°F to 180°F, and at the rate specified.

C.8 Coarse Aggregate

- Stockpiling and loading methods shall permit ready identification of material and minimize segregation and contamination of the aggregate.
- The moisture content of the coarse aggregate shall be below 4% and maintained throughout the project.

- Coarse aggregate shall be spread uniformly without ridges or gaps at the specified rates.
- Spreading of the aggregate shall be adjusted to produce a minimum of excess loose particles, shall provide complete coverage, and there shall be no “excessive” bleed-through after rolling.
- The spreading operation shall be accomplished in such a manner that neither the tires of trucks nor the spreader come into contact with the newly applied bituminous material.

C.9 Material Application Rates

Aggregate Application Rate Pounds per Square Yard

<u>Application Type</u>	<u>Application</u>	<u>Tolerance</u>
Scrub Seal	23	In Field Adjustments

Binder Application Rate Gallons per Square Yard

<u>Application Type</u>	<u>Application</u>	<u>Tolerance</u>
Scrub Seal	0.30 – 0.40	± 0.05

The supplier of the scrub seal binder shall determine the application rate for emulsion and aggregate based on the existing pavement condition and aggregate size. This information shall be reported to the engineer prior to beginning work and shall include an aggregate gradation on the job-specific materials.

C.10 Quality Control

To measure compliance, the contractor shall use the methods described in this section:

- Aggregate Gradation
- Aggregate Moisture Content
- Yield Check on Bituminous Binder
- Temperature Check on Bituminous Binder

If the contractor’s test results exceed any of the identified quality control tolerances, the engineer shall be immediately notified. The engineer will review the explanation and the corrective action taken by the contractor. Another test shall be taken and if the results still exceed the quality control tolerance, placement shall stop. The contractor shall immediately notify the engineer, and identify the cause of the excessive deviation and detail corrective action necessary to bring the deficiency into compliance. The engineer must give approval prior to resuming work.

C.11 Bituminous Binder

The application rate shall not exceed a tolerance of +/-0.05 gallons per square yard from the specified rate, and within the temperature range as specified.

C.12 Course Aggregate

The aggregate shall be clean and uniform, and shall be within the gradation range as specified. Moisture content shall not exceed the tolerance as specified. Apply the aggregates at a minimum rate of 23.0 pounds per square yard. Take all precautions to minimize contamination of the aggregate. All stockpiles will be in place a minimum of 10 calendar days prior to seal coat operations to allow time to sample, test and accept the stockpile.

D Measurement

The department will measure Scrub Seal by the square yard, acceptably completed.

E Payment

The department will pay for measured quantities at the contract unit price under the following bid item:

ITEM NUMBER	DESCRIPTION	UNIT
SPV.0180.002	Scrub Seal	SY

Payment is full compensation for preparing the surface; for furnishing, heating, and applying asphaltic material; for furnishing, drying or moistening, applying, and rolling the cover aggregate; for brooming, finishing, and maintaining the surface.

16. Fog Seal, SPV.0180.003.

A Description

Construct a fog seal surface treatment as described in standard spec 475 and as hereinafter provided.

B Materials

B.1 Asphaltic Material

Provide CSS-1H or CQS-1H, asphaltic material for fog seal. The CSS-1H and CQS-1H, asphaltic material for fog sealing, shall meet the following requirements for the type and grade specified. Only Asphaltic Material supplied from a certified source is approved for use.

Emulsified asphalt compatibility with aggregate shall be verified prior to construction. This shall be done through documentation for material provider.

Emulsified Asphalt

The emulsified asphalt shall meet the requirements of AASHTO M-316 subject to the following modification:

- CSS-1H and CQS-1H shall be produced by using polymer modified base asphalt only. The use of Latex modification shall not be allowed. Any emulsion not meeting this requirement shall not be used.

C Construction

C.1 General

Construct fog seal to the width as shown in the plans or as specified by the engineer. Use asphaltic material of the type and grade the special provisions designate.

C.2 Road Surface Preparation

Cover iron (manholes, gate valve covers, catch basins, sensors, etc.) to prevent adherence of the asphaltic material. Suitable covering includes plywood disks, sand, Kraft paper, roofing felt or other approved methods. Remove the protective coverings before opening the road to traffic.

C.3 Application of Asphaltic Material for Fog Sealing

Fog seal completed chip sealed or scrub sealed area, after sweeping and before placement of permanent pavement markings. Construct the fog seal as follows: Construct a 100 foot test strip. Review the application of diluted (1:1) asphaltic material and adjust the application rate as needed. Apply between 0.07 to 0.18 gallons per square yard, diluted. Apply the fog seal to minimize the amount of overspray. Do not allow traffic on the fog seal until it has cured.

D Measurement

The department will measure Fog Seal by the square yard, acceptably completed.

E Payment

The department will pay for measured quantities at the unit price under the following work items:

ITEM NUMBER	DESCRIPTION	UNIT
SPV.0180.003	Fog Seal	SY

Payment is full compensation for preparing the surface; for furnishing, heating, and applying emulsified asphaltic material; for finishing and maintaining the surface.

ADDITIONAL SPECIAL PROVISION 4

Payment to First-Tier Subcontractors

Within 10 calendar days of receiving a progress payment for work completed by a subcontractor, pay the subcontractor for that work. The prime contractor may withhold payment to a subcontractor if, within 10 calendar days of receipt of that progress payment, the prime contractor provides written notification to the subcontractor and the department documenting "just cause" for withholding payment.

The prime contractor may also withhold routine retainage from payments due subcontractors.

Payment to Lower-Tier Subcontractors

Ensure that subcontracting agreements at all tiers provide prompt payment rights to lower-tier subcontractors that parallel those granted first-tier subcontractors in this provision.

Release of Routine Retainage

After granting substantial completion the department may reduce the routine retainage withheld from the prime contractor to 75 percent of the original total amount retained.

When the Department sends the semi-final estimate the department may reduce the routine retainage withheld from the prime contractor to 10 percent of the original total amount retained.

Within 30 calendar days of receiving the semi-final estimate from the department, submit written certification that subcontractors at all tiers are paid in full for acceptably completed work and that no routine retainage is being withheld. The department will pay the prime contractor in full and reduce the routine retainage withheld from the prime contractor to zero when the department approves the final estimate.

This special provision does not limit the right of the department, prime contractor, or subcontractors at any tier to withhold payment for work not acceptably completed or work subject to an unresolved contract dispute.

ADDITIONAL SPECIAL PROVISION 6

ASP 6 - Modifications to the standard specifications

Make the following revisions to the standard specifications:

104.10.1 General

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Subsection 104.10 specifies a 2-step process for contractors to follow in submitting a cost reduction incentive (CRI) for modifying the contract in order to reduce direct construction costs computed at contract bid prices. The initial submittal is referred to as a CRI concept and the second submittal is a CRI proposal. The contractor and the department will equally share all savings generated to the contract due to a CRI as specified in 104.10.4.2(1). The department encourages the contractor to submit CRI concepts.

104.10.4.2 Payment for the CRI Work

Replace paragraph one with the following effective with the December 2017 letting:

- (1) The department will pay for completed CRI work as specified for progress payments under 109.6. The department will pay for CRI's under the Cost Reduction Incentive administrative item. When all CRI costs are determined, the department will execute a contract change order that does the following:
1. Adjusts the contract time, interim completion dates, or both.
 2. Pays the contractor for the unpaid balance of the CRI work.
 3. Pays the contractor 50 percent of the net savings resulting from the CRI, calculated as follows:

$$NS = CW - CRW - CC - DC$$

Where:

NS = Net Savings

CW = The cost of the work required by the original contract that is revised by the CRI. CW is computed at contract bid prices if applicable.^[1]

CRW = The cost of the revised work, computed at contract bid prices if applicable.^[1]

CC = The contractor's cost of developing the CRI proposal.

DC = The department's cost for investigating, evaluating, and implementing the CRI proposal.

^[1] The department may adjust contract bid prices that, in the engineer's judgement, do not represent the fair value of the work deleted or proposed.

108.11 Liquidated Damages

Replace paragraphs two and three with the following effective with the December 2017 letting:

- (2) This deducted sum is not a penalty but is a fixed, agreed, liquidated damage due the department from the contractor for the added cost of engineering and supervision resulting from the contractor's failure to complete the work within the contract time.
- (3) Unless enhanced in the special provisions, the department will assess the following daily liquidated damages

LIQUIDATED DAMAGES			
ORIGINAL CONTRACT AMOUNT		DAILY CHARGE	
FROM MORE THAN	TO AND INCLUDING	CALENDAR DAY	WORKING DAY
\$0	\$250,000	\$850	\$1700
\$250,000	\$500,000	\$815	\$1630
\$500,000	\$1,000,000	\$1250	\$2500
\$1,000,000	\$2,000,000	\$1540	\$3080
\$2,000,000	—	\$2070	\$4140

203.3.2.2 Removal Operations

Replace the entire text with the following effective with the December 2017 letting:

203.3.2.2.1 General

- (1) Except as specified below for closing culverts, remove the entire top slab of box culverts and the entire superstructure of other culverts and bridges designated for removal. Completely remove existing piles, cribs, or other timber construction within the limits of new embankments, or remove these structures to an elevation at least 2 feet below finished ground line. Remove sidewalls or substructure units in water to an elevation no higher than the elevation of the natural stream or lake bed, or, if grading the channel is required under the contract or the plans, to the proposed finished grade of the stream or lake bed. Remove sidewalls or substructure units not in water down to at least 2 feet below natural or finished ground line.
- (2) If extending or incorporating existing culverts and bridges in the new work, remove only those parts of the existing structure as necessary to provide a proper connection to the new work. Saw, chip, or trim the connecting edges to the required lines and grades without weakening or damaging the remaining part of the structure. During concrete removal, do not damage reinforcing bars left in place as dowels or ties incorporated into the new work.
- (3) Remove pipe culverts designated for salvage in a way that prevents damage to the culverts.
- (4) Dismantle steel structures or parts of steel structures designated for salvage in a way that avoids damage to the members. If the contract specifies removing the structure in a way that leaves it in a condition suitable for re-erection, matchmark members with durable white paint before dismantling. Mark pins, bolts, nuts, loose plates, etc., similarly to indicate their proper location. Paint pins, bolts, pinholes, and machined surfaces with a department-approved rust preventative. Securely wire loose parts to adjacent members, or label and pack them in boxes.
- (5) Remove timber structures or parts of timber structures designated for salvage in a way that prevents damage to the members.
- (6) If the engineer approves, the contractor may temporarily use materials designated for salvage in falsework used to construct new work. Do not damage or reduce the value of those materials through temporary use.

203.3.2.2.2 Deck Removal

- (1) Protect the work as specified in 107.14 during deck removal. Minimize debris falling onto water surfaces and wetlands as the contract specifies in 107.18 or in the special provisions. Also, minimize debris falling on the ground and roadway.
- (2) Do not damage existing bar steel reinforcement, girders, or other components that will be incorporated in new work. Remove decks on prestressed concrete girders using a hydraulic shear or other engineer-approved equipment. Thoroughly clean, realign, and retie reinforcement as necessary.
- (3) After deck removal is complete, notify the engineer to request a damage survey. Point out damage to the engineer. Allow one business day for the engineer to complete the damage survey. If damage is identified, the department will determine if repairs or girder restoration will be allowed.
- (4) If the department allows girder restoration, have a professional engineer registered in the State of Wisconsin analyze the effect of the damage to the bridge, make recommendations, and prepare signed and sealed computations and structural details required to restore girders to their previous structural capacity. Submit the restoration proposal, including analysis and structural details, to the department and design engineer of record. The department will accept or reject the restoration proposal within 3 business days. Do not begin restoration work until the department allows in writing.
- (5) The engineer will not extend contract time to assess or remediate contractor caused damage.

203.5.1 General

Replace paragraph two with the following effective with the December 2017 letting:

- (2) Payment is full compensation for breaking down and removing; costs associated with contractor-caused damage; required salvaging, storing, and disposing of materials; and, unless the contract specifies granular backfill, for backfilling.

415.2.3 Expansion Joint Filler

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Furnish expansion joint filler conforming to AASHTO M153, AASHTO M213, or ASTM D8139 in lengths equal to the pavement lane width and of the thickness and height the plans show. Where dowel bars are required, use filler with factory-punched holes at the dowel bar locations and with a diameter not greater than 1/8 inch larger than the nominal dowel bar diameter.
-

415.3.20 Filling Joints

Replace paragraph two with the following effective with the December 2017 letting:

- (2) Clean joints of laitance, curing compound, and other contaminants before filling. Saw construction joints at least 3/4 inches deep before filling. Sawing is not required for tooled joints in curb and gutter. Sandblast or waterblast exposed joint faces using multiple passes as required to clean joint surfaces of material that might prevent bonding. Blow clean and dry with oil-free compressed air immediately before filling.
-

415.5.1 General

Replace paragraph six with the following effective with the December 2017 letting:

- (6) Payment for Concrete Pavement Joint Filling is full compensation for filling concrete pavement joints; filling adjacent curb and gutter joints; and for sawing.
-

440.3.4.2 Contractor Testing

Replace paragraph two with the following effective with the December 2017 letting:

- (2) Coordinate with the engineer at least 24 hours before making profile runs for acceptance unless the engineer approves otherwise. The department may require testing to accommodate staged construction or if corrective action is required.
-

455.5.3 Tack Coat

Replace paragraph two with the following effective with the December 2017 letting:

- (2) The department will adjust pay for Tack Coat, under the Nonconforming Tack Coat administrative item, for nonconforming material the engineer allows to remain in place at a maximum of 75 percent of the contract unit price.
-

460.2.7 HMA Mixture Design

Replace paragraph one with the following effective with the December 2017 letting:

- (1) For each HMA mixture type used under the contract, develop and submit an asphaltic mixture design according to CMM 8-66 and conforming to the requirements of table 460-1 and table 460-2. The values listed are design limits; production values may exceed those limits. The department will review mixture designs and report the results of that review to the designer according to CMM 8-66.

TABLE 460-2 MIXTURE REQUIREMENTS

Mixture type	LT	MT	HT	SMA
ESALs x 10 ⁶ (20 yr design life)	<2.0	2 - <8	>8	—
LA Wear (AASHTO T96)				
100 revolutions(max % loss)	13	13	13	13
500 revolutions(max % loss)	50	45	45	40
Soundness (AASHTO T104) (sodium sulfate, max % loss)	12	12	12	12
Freeze/Thaw (AASHTO T103) (specified counties, max % loss)	18	18	18	18
Fractured Faces (ASTM D5821) (one face/2 face, % by count)	65/—	75 / 60	98 / 90	100/90
Flat & Elongated (ASTM D4791) (max %, by weight)	5 (5:1 ratio)	5 (5:1 ratio)	5 (5:1 ratio)	20 (3:1 ratio)
Fine Aggregate Angularity (AASHTO T304, method A, min)	40	43	45	45
Sand Equivalency (AASHTO T176, min)	40	40	45	50
Gyratory Compaction				
Gyrations for N _{ini}	6	7	8	8
Gyrations for N _{des}	40	75	100	65
Gyrations for N _{max}	60	115	160	160
Air Voids, %V _a (%G _{mm} N _{des})	4.0 (96.0)	4.0 (96.0)	4.0 (96.0)	4.0 (96.0)
% G _{mm} N _{ini}	<= 91.5 ^[1]	<= 89.0 ^[1]	<= 89.0	—
% G _{mm} N _{max}	<= 98.0	<= 98.0	<= 98.0	—
Dust to Binder Ratio ^[2] (% passing 0.075/P _{be})	0.6 - 1.2	0.6 - 1.2	0.6 - 1.2	1.2 - 2.0
Voids filled with Binder (VFB or VFA, %)	68 - 80 ^[4] [5]	65 - 75 ^[3] [5]	65 - 75 ^[3] [5]	70 - 80
Tensile Strength Ratio (TSR) (AASHTO T283) ^[6] [7]				
no antistripping additive	0.75 min	0.75 min	0.75 min	0.75 min
with antistripping additive	0.80 min	0.80 min	0.80 min	0.80 min
Draindown (AASHTO T305) (%)	—	—	—	0.30

^[1] The percent maximum density at initial compaction is only a guideline.

^[2] For a gradation that passes below the boundaries of the caution zone (ref. AASHTO M323), the dust to binder ratio limits are 0.6 - 1.6.

^[3] For No. 5 (9.5mm) and No. 4 (12.5 mm) nominal maximum size mixtures, the specified VFB range is 70 - 76 percent.

^[4] For No. 2 (25.0mm) nominal maximum size mixes, the specified VFB lower limit is 67 percent.

^[5] For No. 1 (37.5mm) nominal maximum size mixes, the specified VFB lower limit is 67 percent.

^[6] WisDOT eliminates freeze-thaw conditioning cycles from the TSR test procedure.

^[7] Run TSR at asphalt content corresponding to 3.0% air void regressed design using distilled water for testing.

460.2.8.2.1.3.1 Contracts with 5000 Tons of Mixture or Greater

Replace paragraph six with the following:

- (6) Conduct TSR tests during mixture production according to CMM 8-36.6.14. Test each full 50,000 ton production increment, or fraction of an increment, after the first 5000 tons of production. Perform required increment testing in the first week of production of that increment. If production TSR values are below the limit specified in CMM 8-36.6.14, notify the engineer. The engineer and contractor will jointly determine a corrective action.

502.2.7 Preformed Joint Filler

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Use preformed joint filler conforming to AASHTO M153, AASHTO M213, or ASTM D8139.
-

502.3.7.8 Floors

Replace paragraph fourteen with the following effective with the December 2017 letting:

- (14) Unless specified otherwise, transversely tine finish the floors of structures with approach pavements designed for speeds of 40 mph or greater as specified in 415.3.8.3, except make the tining 1/8 inch in depth and do not perform tining within 12 inches of gutters. The contractor may apply a broom finish, described below, instead of the artificial turf drag finish required before tining. The contractor may perform tining manually, if it obtains a finish satisfactory to the engineer. Perform tining within 20 degrees of the centerline of bearing of the substructure units on bridge decks having skew angles of 20 degrees or greater.
-

614.2.1 General

Add the following as paragraph ten effective with the December 2017 letting:

- (10) Furnish guardrail reflectors from the department's APL.
-

614.3.2.1 Installing Posts

Add the following as paragraph five effective with the December 2017 letting:

- (5) Provide post-mounted reflectors every 100 feet with one at the beginning and end of each run and a minimum of three reflectors per run.
-

614.5 Payment

Replace paragraph four with the following effective with the December 2017 letting:

- (4) Payment for the Steel Thrie Beam, Steel Plate Beam Guard, Guardrail Stiffened, MGS Guardrail, Short Radius, and various transition bid items is full compensation for providing guardrail and transitions including post-mounted reflectors; for repairing damaged zinc coatings; and for excavating, backfilling, and disposing of surplus material.
-

641.2.9 Overhead Sign Supports

Replace paragraph three with the following effective with the December 2017 letting:

- (3) Provide steel pole shafts, mast arms or trusses, and luminaire arms zinc coated according to ASTM A123. The contractor may provide either straight or tapered pole and arm shafts unless the plans specify otherwise. Provide bolts and other hardware conforming to 641.2.2.
-

642.2.2.1 General

Replace the entire text with the following effective with the December 2017 letting:

- (1) Provide each field office with two rooms, separated by an interior door with a padlock. Ensure that each room has a separate exterior door and its own air conditioner. Locate the office where a quality internet connection can be achieved.
- (2) Provide long distance telephone service via a land line for exclusive department use that has the following:
- Two programmable touch-tone phones, one of which is cordless. Ensure that phone operations will not interfere with other telecommunications equipment.
 - Voice mail service or an answering machine.
- (3) Provide high-speed internet service for exclusive department use via cable or DSL connection with a modem/router and capable of supporting cloud enabled file sharing, voice over internet protocol (VoIP), video conferencing, and web based applications. Ensure that system meets the following:
- Includes a wireless network for the field office.

- Can accommodate IPsec based VPN products.
 - Has a bandwidth range as follows:
 - Field office with 1-5 staff: A minimum connection speed of 5 Mbps download and 1 Mbps upload. If a cable or DSL option is not available the contractor may provide a personal hotspot using cell phone tethering or other device able to achieve the specified minimum speeds inside the field office.
 - Field office with 6 or more staff: A minimum connection speed of 10 Mbps + 1/2 Mbps per user download and 5 Mbps upload.
 - Projects over 500 million dollars: A minimum connection speed of 20 Mbps + 1/2 Mbps per user download and 10 Mbps upload. Coordinate network setup at the leased office with the WisDOT network team.
- (4) Provide and maintain a Windows 7 and Windows 10 compliant multi-function device with copy, print, and scan capabilities that can accommodate both 8 1/2" x 11" and 11" x 17" paper. Replenish paper, toner cartridges, and other supplies before fully expended. Ensure that department staff can connect to the device either directly or through the field office wireless network.
- (5) Equip with a drafting table with a drafter's stool. Except as specified in 642.2.2.4, provide 2 ergonomically correct office chairs in working condition with, at a minimum, the following:
1. Five-legged base with casters.
 2. Seat adjustable from 15 to 22 inches from the floor with a seamless waterfall, rounded, front edge.
 3. High backrest with no arms or adjustable arms.

643.3.1 General

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Provide and maintain traffic control devices located where the plans show or engineer directs to maintain a safe work zone throughout the contract duration. Relocate as required to accommodate changing work operations. When not in use, place devices away from traffic outside of paved and gravel shoulder surfaces. Where there is barrier on the shoulder, the contractor may place devices not in use on the shoulder as close as possible to the barrier and delineated with drums. Lay signs and supports flat on the grade with uprights oriented parallel to and downstream from traffic. Do not stack devices or equipment. Promptly remove temporary devices from within the project limits as follows:
- That will not be used within 14 consecutive calendar days.
 - Within 5 business days of substantial completion unless the engineer allows otherwise.

645.2.2.2 Geotextile, Type SAS (Subgrade Aggregate Separation)

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Furnish fabric conforming to the following physical properties:

TEST	METHOD	VALUE ^[1]
Minimum grab tensile strength	ASTM D4632	170 lb
Minimum puncture strength	ASTM D6241	350 lb
Maximum apparent opening size	ASTM D4751	No. 70
Minimum permittivity	ASTM D4491	0.35 s ⁻¹

^[1] All numerical values represent minimum/maximum average roll values. Average test results from all rolls in a lot must conform to the tabulated values.

645.2.2.4 Geotextile, Type DF (Drainage Filtration)

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Furnish fabric conforming with the physical requirements of either schedule A, schedule B, or schedule C as the contract specifies.

SCHEDULE A TEST	METHOD	VALUE ^[1]
Minimum grab tensile strength	ASTM D4632	110 lb
Minimum puncture strength	ASTM D6241	200 lb
Minimum apparent breaking elongation	ASTM D4632	30%

Maximum apparent opening size	ASTM D4751	300 μm
Minimum permittivity	ASTM D4491	0.70 s^{-1}
SCHEDULE B TEST	METHOD	VALUE ^[1]
Minimum grab tensile strength	ASTM D4632	180 lb
Minimum puncture strength	ASTM D6241	350 lb
Minimum apparent breaking elongation	ASTM D4632	30%
Maximum apparent opening size	ASTM D4751	300 μm
Minimum permittivity	ASTM D4491	1.35 s^{-1}
SCHEDULE C TEST	METHOD	VALUE ^[1]
Minimum grab tensile strength	ASTM D4632	180 lb
Minimum puncture strength	ASTM D6241	350 lb
Minimum apparent breaking elongation	ASTM D4632	15%
Maximum apparent opening size	ASTM D4751	600 μm
Minimum permittivity	ASTM D4491	1.00 s^{-1}

^[1] All numerical values represent minimum/maximum average roll values. Average test results from all rolls in a lot must conform to the tabulated values.

645.2.2.6 Geotextile, Type R (Riprap)

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Use fabric conforming to the following physical properties:

TEST	METHOD	VALUE ^[1]
Minimum grab tensile strength	ASTM D4632	205 lb
Minimum puncture strength	ASTM D6241	400 lb
Minimum apparent breaking elongation	ASTM D4632	15%
Maximum apparent opening size	ASTM D4751	No. 30
Minimum permittivity	ASTM D4491	0.12 s^{-1}

^[1] All numerical values represent minimum/maximum average roll values. Average test results from all rolls in a lot must conform to the tabulated values.

645.2.2.7 Geotextile, Type HR (Heavy Riprap)

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Use fabric conforming to the following physical properties:

TEST	METHOD	VALUE ^[1]
Minimum grab tensile strength, lb	ASTM D4632	305 lb
Minimum puncture strength, lb	ASTM D6241	500 lb
Minimum apparent breaking elongation, %	ASTM D4632	15%
Maximum apparent opening size	ASTM D4751	No. 30
Minimum permittivity	ASTM D4491	0.40, s^{-1}

^[1] All numerical values represent minimum/maximum average roll values. Average test results from all rolls in a lot must conform to the tabulated values.

645.2.2.8 Geotextile, Type C (Modified SAS)

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Use fabric conforming to the following physical properties:

TEST	METHOD	VALUE ^[1]
Grab tensile strength, lb	ASTM D4632	205 lb
Puncture strength, lb	ASTM D6241	350 lb
Maximum apparent opening size	ASTM D4751	No. 50
Minimum permittivity	ASTM D4491	0.12 s^{-1}

^[1] All numerical values represent minimum/maximum average roll values. Average test results from all rolls in a lot must conform to the tabulated values.

646.3.1.1 General Marking

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Prepare the surface and apply marking as the manufacturer specifies. Provide manufacturer specifications as the engineer requests. Do not mark over a marking product with less adherence or over chipped or peeled marking. Do not remove polymer overlay materials in areas receiving pavement marking. Use only epoxy pavement marking where the contract requires marking placed on polymer overlays.

Replace paragraph five with the following effective with the December 2017 letting:

- (5) After the marking can sustain exposure to traffic, re-apply clear protective surface treatment conforming to 502.2.11 where removed from structures during marking surface preparation. Seal exposed concrete including grooves for tape. Cover marking during resealing with a system that will not degrade the marking's retroreflectivity when removed. Uncover marking before opening to traffic.

701.3 Contractor Testing

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Perform contract required QC tests for samples randomly located according to CMM 8-30. Also perform other tests as necessary to control production and construction processes, and additional testing enumerated in the contractor's quality control plan or that the engineer directs. Use test methods as follows:

TABLE 701-2 TESTING STANDARDS

TEST	TEST STANDARD
Washed P 200 analysis	AASHTO T11 ^[1]
Sieve analysis of fine and coarse aggregate	AASHTO T27 ^[1]
Aggregate moisture	AASHTO T255 ^[1]
Sampling freshly mixed concrete	AASHTO R60
Air content of fresh concrete	AASHTO T152 ^[2]
Air void system of fresh concrete	AASHTO Provisional Standard TP118
Concrete slump	AASHTO T119 ^[2]
Concrete temperature	ASTM C1064
Concrete compressive strength	AASHTO T22
Making and curing concrete cylinders	AASHTO T23
Standard moist curing for concrete cylinders	AASHTO M201

^[1] As modified in CMM 8-60.

^[2] As modified in CMM 8-70.

715.2.3.1 Pavements

Add the following as paragraph six effective with the December 2017 letting:

- (6) For new lab-qualified mixes, test the air void system of the proposed concrete mix conforming to AASHTO provisional standard TP 118. Include the SAM number as a part of the mix design submittal.

715.3.1.1 General

Replace paragraph one with the following effective with the December 2017 letting:

- (1) Provide slump, air content, concrete temperature and compressive strength test results as specified in 710.5. Provide a battery of QC tests, consisting of results for each specified property, using a single sample randomly located within each subplot. Cast three cylinders for strength evaluation. For pavement concrete, also test the air void system conforming to AASHTO provisional standard TP118 at least once per lot and enter the SAM number in the MRS for information only.

715.3.1.3 Department Verification Testing

Replace paragraph one with the following effective with the December 2017 letting:

- (1) The department will perform verification testing as specified in 701.4.2 with additional testing as required to obtain at least 1 verification test per lot for air content, slump, temperature, and compressive strength.

Errata

Make the following corrections to the standard specifications:

106.3.3.1 General

Correct errata by changing "acceptance" to "approval".

- (1) For manufactured products or assemblies, the department may base approval on a product certification or require both a product certification and production plant certification.

205.3.1 General

Correct errata by replacing paragraphs three and four with the following to reflect current practice to incorporate suitable materials.

- (3) Replace unsuitable material with satisfactory material. Trim and finish the roadway. Maintain the work done under 205 in a finished condition until acceptance.

521.2 Materials

Correct errata by deleting bullet three and including aluminum coated pipe in bullet one.

- (1) Furnish corrugated steel pipe and steel apron end walls as follows:
 - Corrugated steel culvert pipe, steel apron endwalls, aluminum coated corrugated steel culvert pipe, and other components conforming to AASHTO M36.
 - Polymer coated corrugated steel culvert pipe and pipe arch fabricated from zinc coated sheet steel conforming to AASHTO M218. Before fabrication, coat the sheets on both sides with polymer protective coating grade 250/250 according to AASHTO M246. Fabricate the pipe according to AASHTO M245.

614.3.2.2 Installing Rail

Correct errata for splice location and allow punching or drilling holes and slots.

- (1) Install rail with lap splices in the direction of traffic. Ensure that the number and dimensions of holes and bolts conforms to the plan details for new splices. Place the round head of bolts on the traffic side.
- (2) Cut rails to length by shearing or sawing; do not use cutting torches. Drill or punch bolt holes and slots; ensure that they are burr free. After installation, cut anchor bolts that project more than one inch from the nut to 1/2 inch from the nut; deburr the threaded end of cut bolts.

618.1 Description

Correct errata by deleting designated detours from the scope of Maintenance and Repair of Haul Roads.

- (1) This section describes maintaining, repairing, and restoring all public roads, streets, drainage facilities, and other components used for hauling by contractor, subcontractor, or supplier to support work for a department contract to its pre-haul condition. Public roads and streets shall be limited to those not a part of the State Trunk Highway System and from now on called haul roads.

646.3.1.2 Liquid Marking

Correct errata by changing "epoxy overlays" to "polymer overlays".

- (5) Apply liquid marking and glass beads across the line at or exceeding the following:

LIQUID MARKING	PAVEMENT TYPE	THICKNESS (mils)	BEAD APPLICATION (pounds per gallon)
Paint	all	16	8-10
Epoxy	SMA, seal coats, and polymer overlays	25	25
Epoxy	all other	20	22.5

ADDITIONAL SPECIAL PROVISION 7

A. Reporting 1st Tier and DBE Payments During Construction

1. Comply with reporting requirements specified in the department's Civil Rights Compliance, Contractor's User Manual, Sublets and Payments.
2. Report payments to all DBE firms within 10 calendar days of receipt of a progress payment by the department or a contractor for work performed, materials furnished, or materials stockpiled by a DBE firm. Report the payment as specified in A(1) for all work satisfactorily performed and for all materials furnished or stockpiled.
3. Report payments to all first tier subcontractor relationships within 10 calendar days of receipt of a progress payment by the department for work performed. Report the payment as specified in A(1) for all work satisfactorily performed.
4. All tiers shall report payments as necessary to comply with the DBE payment requirement as specified in A(2).
5. Require all first tier relationships, DBE firms and all other tier relationships necessary to comply with the DBE payment requirement in receipt of a progress payment by contractor to acknowledge receipt of payment as specified in A(1), (2), (3) and (4).
6. All agreements made by a contractor shall include the provisions in A(1), (2), (3), (4) and (5), and shall be binding on all first tier subcontractor relationships and all contractors and subcontractors utilizing DBE firms on the project.

B. Costs for conforming to this special provision are incidental to the contract.

NOTE: CRCS Prime Contractor payment is currently not automated and will need to be manually loaded into the Civil Rights Compliance System. Copies of prime contractor payments received (check or ACH) will have to be forwarded to paul.ndon@dot.wi.gov within 5 days of payment receipt to be logged manually.

***Additionally, for information on Subcontractor Sublet assignments, Subcontractor Payments and Payment Tracking, please refer to the CRCS Payment and Sublets manual at:

<http://wisconsindot.gov/Documents/doing-bus/civil-rights/labornwage/crcs-payments-sublets-manual.pdf>

ADDITIONAL SPECIAL PROVISION 9-S
Electronic Labor Data Submittal for
State Funded Only Projects

(1) Use the Workforce Utilization Report Microsoft Excel spread sheet, or other compatible spread sheet (i.e., Google Spread Sheet), to report required labor data. Details and the Excel spreadsheet are available online through the department's highway construction contract information (HCCI) site on the Labor, Wages, and EEO Information page at:

<http://wisconsindot.gov/Pages/doing-bus/civil-rights/labornwage/default.aspx>

(2) Ensure that all tiers of subcontractors, including all trucking firms, submit their labor data electronically via the Excel spread sheet to the prime contractor within 14 calendar days of the end of each quarter (quarters are defined as January-March, April-June, July-September, and October-December). The prime contractor shall coordinate collection of their subcontractors' spread sheets and forward them to the Regional Labor Compliance Specialist within 21 calendar days of the end of each quarter. Every company or contractor providing physical labor towards completing the project is a subcontractor under this special provision.

(3) Upon receipt of contract execution, promptly make all affected companies or contractors aware of the requirements under this special provision and arrange for them to receive an Excel spreadsheet as part of their subcontract documents.

(4) The department will reject all paper submittals of information required under this special provision. All costs for conforming to this special provision are incidental to the contract.

Non-discrimination Provisions

During the performance of this contract, the contractor, for itself, its assignees, and successors in interest (hereinafter referred to as the "contractor") agrees as follows:

1. Compliance with Regulations: The contractor (hereinafter includes consultants) will comply with the Acts and the Regulations relative to Non-discrimination in Federally-assisted programs of the U.S. Department of Transportation, Federal Highway Administration, as they may be amended from time to time, which are herein incorporated by reference and made a part of this contract.

2. Non-discrimination: The contractor, with regard to the work performed by it during the contract, will not discriminate on the grounds of race, color, or national origin in the selection and retention of subcontractors, including procurements of materials and leases of equipment. The contractor will not participate directly or indirectly in the discrimination prohibited by the Acts and the Regulations, including employment practices when the contract covers any activity, project, or program set forth in Appendix B of 49 CFR Part 21.

3. Solicitations for Subcontracts, Including Procurements of Materials and Equipment: In all solicitations, either by competitive bidding, or negotiation made by the contractor for work to be performed under a subcontract, including procurements of materials, or leases of equipment, each potential subcontractor or supplier will be notified by the contractor of the contractor's obligations under this contract and the Acts and the Regulations relative to Non-discrimination on the grounds of race, color, or national origin.

4. Information and Reports: The contractor will provide all information and reports required by the Acts, the Regulations, and directives issued pursuant thereto and will permit access to its books, records, accounts, other sources of information, and its facilities as may be determined by the Recipient or the Federal Highway Administration to be pertinent to ascertain compliance with such Acts, Regulations, and instructions. Where any information required of a contractor is in the exclusive possession of another who fails or refuses to furnish the information, the contractor will so certify to the Recipient or the Federal Highway Administration, as appropriate, and will set forth what efforts it has made to obtain the information.

5. Sanctions for Noncompliance: In the event of a contractor's noncompliance with the Non-discrimination provisions of this contract, the Recipient will impose such contract sanctions as it or the Federal Highway Administration may determine to be appropriate, including, but not limited to:

- a. Withholding payments to the contractor under the contract until the contractor complies; and/or
- b. Cancelling, terminating, or suspending a contract, in whole or in part.

6. Incorporation of Provisions: The contractor will include the provisions of paragraphs one through six in every subcontract, including procurements of materials and leases of equipment, unless exempt by the Acts, the Regulations and directives issued pursuant thereto. The contractor will take action with respect to any subcontract or procurement as the Recipient or the Federal Highway Administration may direct as a means of enforcing such provisions including sanctions for noncompliance. Provided, that if the contractor becomes involved in, or is threatened with litigation by a subcontractor, or supplier because of such direction, the contractor may request the Recipient to enter into any litigation to protect the interests of the Recipient. In addition, the contractor may request the United States to enter into the litigation to protect the interests of the United States.

During the performance of this contract, the contractor, for itself, its assignees, and successors in interest (hereinafter referred to as the "contractor") agrees to comply with the following non-discrimination statutes and authorities; including but not limited to:

Pertinent Non-Discrimination Authorities:

- Title VI of the Civil Rights Act of 1964 (42 U.S.C. § 2000d et seq., 78 stat. 252), (prohibits discrimination on the basis of race, color, national origin); and 49 CFR Part 21.
- The Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970, (42 U.S.C. § 4601), (prohibits unfair treatment of persons displaced or whose property has been acquired because of Federal or Federal-aid programs and projects);
- Federal-Aid Highway Act of 1973, (23 U.S.C. § 324 et seq.), (prohibits discrimination on the basis of sex);
- Section 504 of the Rehabilitation Act of 1973, (29 U.S.C. § 794 et seq.), as amended, (prohibits discrimination on the basis of disability); and 49 CFR Part 27;
- The Age Discrimination Act of 1975, as amended, (42 U.S.C. § 6101 et seq.), (prohibits discrimination on the basis of age);
- Airport and Airway Improvement Act of 1982, (49 USC § 471, Section 47123), as amended, (prohibits discrimination based on race, creed, color, national origin, or sex);
- The Civil Rights Restoration Act of 1987, (PL 100-209), (Broadened the scope, coverage and applicability of Title VI of the Civil Rights Act of 1964, The Age Discrimination Act of 1975 and Section 504 of the Rehabilitation Act of 1973, by expanding the definition of the terms "programs or activities" to include all of the programs or activities of the Federal-aid recipients, sub-recipients and contractors, whether such programs or activities are Federally funded or not);
- Titles II and III of the Americans with Disabilities Act, which prohibit discrimination on the basis of disability in the operation of public entities, public and private transportation systems, places of public accommodation, and certain testing entities (42 U.S.C. §§ 12131-12189) as implemented by Department of Transportation regulations at 49 C.F.R. parts 37 and 38;
- The Federal Aviation Administration's Non-discrimination statute (49 U.S.C. § 47123) (prohibits discrimination on the basis of race, color, national origin, and sex);

- Executive Order 12898, Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations, which ensures Non-discrimination against minority populations by discouraging programs, policies, and activities with disproportionately high and adverse human health or environmental effects on minority and low-income populations;
- Executive Order 13166, Improving Access to Services for Persons with Limited English Proficiency, and resulting agency guidance, national origin discrimination includes discrimination because of Limited English proficiency (LEP). To ensure compliance with Title VI, you must take reasonable steps to ensure that LEP persons have meaningful access to your programs (70 Fed. Reg. at 74087 to 74100);
- Title IX of the Education Amendments of 1972, as amended, which prohibits you from discriminating because of sex in education programs or activities (20 U.S.C. 1681 et seq).

Effective August 2015 letting

BUY AMERICA PROVISION

All steel and iron materials permanently incorporated in this project shall be domestic products and all manufacturing and coating processes for these materials from smelting forward in the manufacturing process must have occurred within the United States. Coating includes epoxy coating, galvanizing, painting and any other coating that protects or enhances the value of a material subject to the requirements of Buy America. The exemption of this requirement is the minimal use of foreign materials if the total cost of such material permanently incorporated in the product does not exceed one-tenth of one percent (1/10 of 1%) of the total contract cost or \$2,500.00, whichever is greater. For purposes of this paragraph, the cost is that shown to be the value of the subject products as they are delivered to the project. The contractor shall take actions and provide documentation conforming to CMM 2-28.5 to ensure compliance with this "Buy America" provision.

<http://wisconsindot.gov/rdwy/cmm/cm-02-28.pdf>

Upon completion of the project certify to the engineer, in writing using department form WS4567, that all steel, iron, and coating processes for steel or iron incorporated into the contract work conform to these "Buy America" provisions. Attach a list of exemptions and their associated costs to the certification form. Department form WS4567 is available at:

<http://wisconsindot.gov/hcciDocs/contracting-info/ws4567.doc>

March 2017

**NOTICE TO BIDDERS
WAGE RATE DECISION**

The wage rate decision of the Department of Labor which has been incorporated in these advertised specifications is incomplete in that the classifications may be omitted from the Department of Labor's decision.

Since the bidder is responsible, independently, for ascertaining area practice with respect to the necessity, or lack of necessity, for the use of these classifications in the prosecution of the work contemplated by this project, no inference may be drawn from the omission of these classifications concerning prevailing area practices relative to their use. Further, this omission will not, per se, be construed as establishing any governmental liability for increased labor cost if it is subsequently determined that such classifications are required.

There may be omissions and/or errors in the federal wage rates. The bidder is responsible for evaluating and determining the correct applicable rate.

If a project includes multiple types of construction (highway, bridge over navigable water, sanitary sewer and water main, building) and there is not a separate wage determination for this type of work included in the proposal, use the wage determination that is in the proposal.



Proposal Schedule of Items

Page 1 of 3

Proposal ID: 20180213023 Project(s): 1000-18-06

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0002	213.0100 Finishing Roadway (project) 01. 1000-18-06	1.000 EACH	_____.	_____.
0004	618.0100 Maintenance And Repair of Haul Roads (project) 01. 1000-18-06	1.000 EACH	_____.	_____.
0006	619.1000 Mobilization	1.000 EACH	_____.	_____.
0008	628.7015 Inlet Protection Type C	40.000 EACH	_____.	_____.
0010	634.0612 Posts Wood 4x6-Inch X 12-FT	4.000 EACH	_____.	_____.
0012	634.0614 Posts Wood 4x6-Inch X 14-FT	326.000 EACH	_____.	_____.
0014	634.0616 Posts Wood 4x6-Inch X 16-FT	284.000 EACH	_____.	_____.
0016	634.0618 Posts Wood 4x6-Inch X 18-FT	33.000 EACH	_____.	_____.
0018	637.2210 Signs Type II Reflective H	3,233.690 SF	_____.	_____.
0020	637.2215 Signs Type II Reflective H Folding	10.360 SF	_____.	_____.
0022	637.2230 Signs Type II Reflective F	1,168.740 SF	_____.	_____.
0024	638.2102 Moving Signs Type II	1.000 EACH	_____.	_____.
0026	638.2602 Removing Signs Type II	585.000 EACH	_____.	_____.
0028	638.3000 Removing Small Sign Supports	622.000 EACH	_____.	_____.
0030	643.0410 Traffic Control Barricades Type II	20.000 DAY	_____.	_____.
0032	643.0705 Traffic Control Warning Lights Type A	40.000 DAY	_____.	_____.



Proposal Schedule of Items

Page 2 of 3

Proposal ID: 20180213023 Project(s): 1000-18-06

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0034	643.0900 Traffic Control Signs	20.000 DAY	_____.	_____.
0036	643.5000 Traffic Control	1.000 EACH	_____.	_____.
0038	646.1005 Marking Line Paint 4-Inch	597,600.000 LF	_____.	_____.
0040	646.1020 Marking Line Epoxy 4-Inch	136,200.000 LF	_____.	_____.
0042	646.3005 Marking Line Paint 8-Inch	5,550.000 LF	_____.	_____.
0044	646.5020 Marking Arrow Epoxy	6.000 EACH	_____.	_____.
0046	646.5120 Marking Word Epoxy	5.000 EACH	_____.	_____.
0048	646.6120 Marking Stop Line Epoxy 18-Inch	137.000 LF	_____.	_____.
0050	646.7120 Marking Diagonal Epoxy 12-Inch	45.000 LF	_____.	_____.
0052	646.7220 Marking Chevron Epoxy 24-Inch	103.000 LF	_____.	_____.
0054	646.7420 Marking Crosswalk Epoxy Transverse Line 6-Inch	858.000 LF	_____.	_____.
0056	646.8020 Marking Corrugated Median Epoxy	88.000 SF	_____.	_____.
0058	646.8120 Marking Curb Epoxy	375.000 LF	_____.	_____.
0060	646.8220 Marking Island Nose Epoxy	5.000 EACH	_____.	_____.
0062	648.0100 Locating No-Passing Zones	47.790 MI	_____.	_____.
0064	649.0105 Temporary Marking Line Paint 4-Inch	242,000.000 LF	_____.	_____.



Proposal Schedule of Items

Page 3 of 3

Proposal ID: 20180213023 Project(s): 1000-18-06

Federal ID(s): N/A

SECTION: 0001

Contract Items

Alt Set ID:

Alt Mbr ID:

Proposal Line Number	Item ID Description	Approximate Quantity and Units	Unit Price	Bid Amount
0066	649.2102 Temporary Raised Pavement Markers Type II	5,650.000 EACH	_____.	_____.
0068	SPV.0180 Special 001. CHIP SEAL	261,000.000 SY	_____.	_____.
0070	SPV.0180 Special 002. SCRUB SEAL	542,000.000 SY	_____.	_____.
0072	SPV.0180 Special 003. FOG SEAL	853,000.000 SY	_____.	_____.
Section: 0001			Total:	_____.
			Total Bid:	_____.

PLEASE ATTACH SCHEDULE OF ITEMS HERE