NOVEMBER 2017

ORDER OF SHEETS

Section No. 1 Section No. 2 Typical Sections and Details Section No. 3 Estimate of Quantities Miscellaneous Quantitles

Right of Way Plat Section No. 5 Plan and Profile Standard Detail Drawings

Sign Plates Structure Plans

Cross Sections

MARATHON COUNTY

TOTAL SHEETS - 120

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

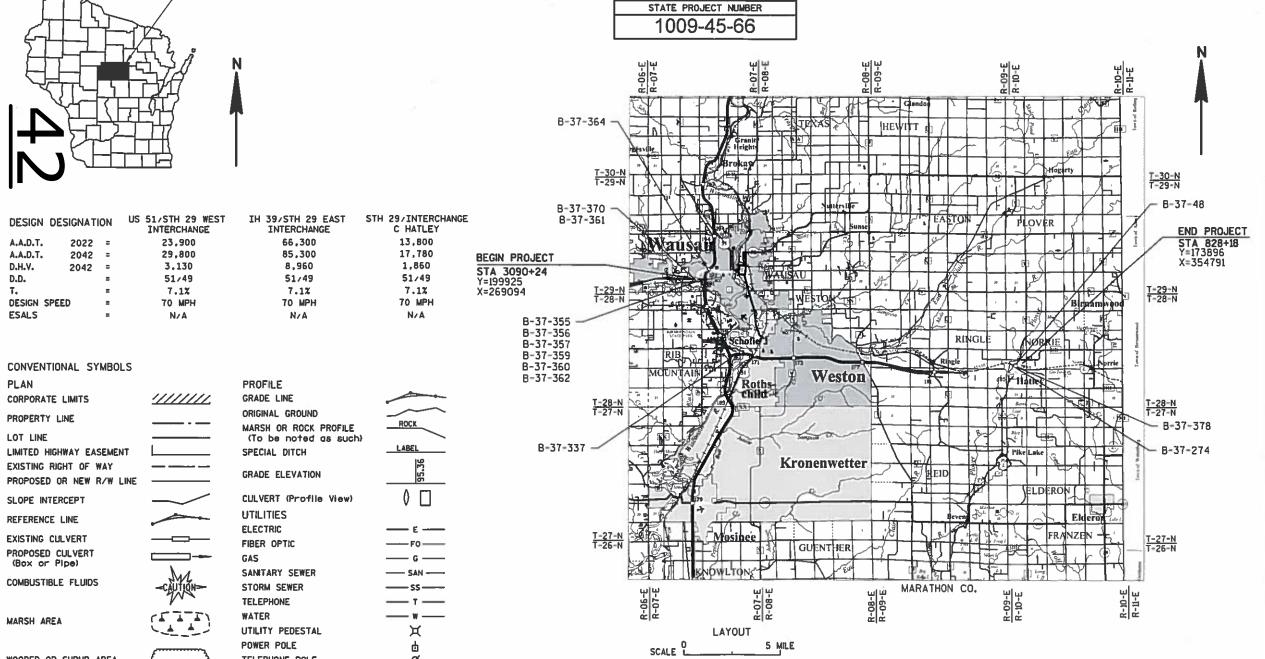
FEDERAL PROJECT STATE PROJECT **PROJECT** CONTRACT 1009-45-66 WISC 2017620

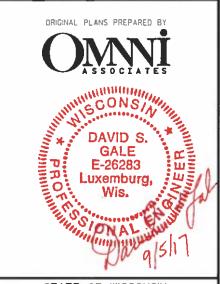
VARIOUS STRUCTURES MARATHON COUNTY

STRUCTURES PER PROVIDED LIST

STH 29

MARATHON COUNTY





STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PREPARED BY OMNNI ASSOCIATES Regional Examiner Regional Supervisor.

APPROVED FOR THE DEPARTMENT 9/5/17

FILE NAME : F:\TR\JOBS\E2154A15\CIVIL 3D 2014\SHEETSPLAN\10094566-010101-TI.DWG LAYOUT NAME - TITLE SHEET 1 IN EO 0.5 MI

TELEPHONE POLE

WOODED OR SHRUB AREA

PLOT DATE : 9/5/2017 2:42 PM

TOTAL NET LENGTH OF CENTERLINE = 0.000

PLOT BY : MATT TOMSOVIC

PLOT NAME :

2

2

GENERAL NOTES

THERE ARE NO KNOWN UTILITY FACILITIES WITHIN THE PROJECT AREA. HOWEVER, IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THIS.



OTHER CONTACTS

DNR LIAISON

DEPARTMENT OF NATURAL RESOURCES

473 GRIFFITH DRIVE

WI SCONSI N RAPI DS, WI 54494 ATTN: MARC HERSHFI ELD TELEPHONE: 715-421-7867

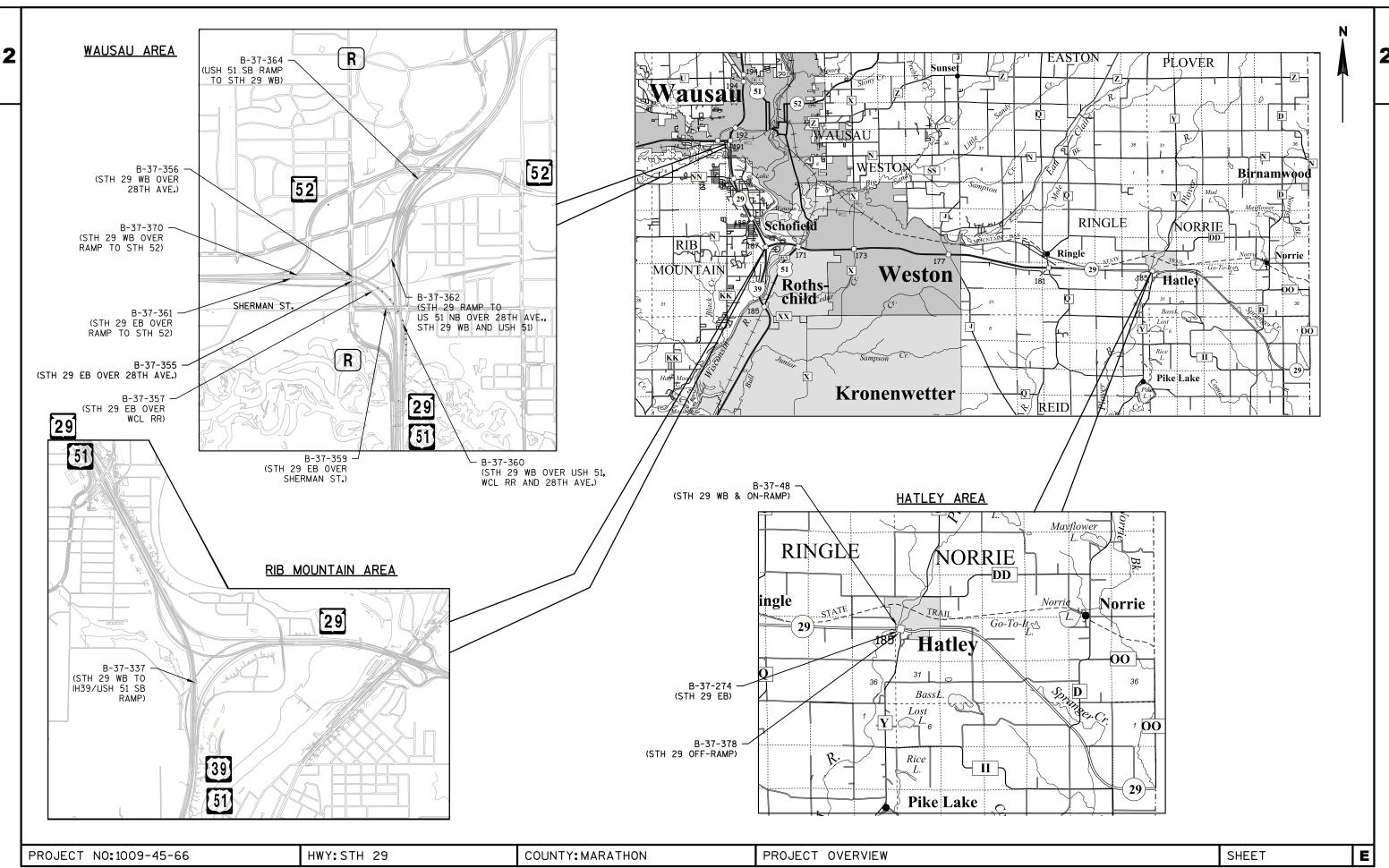
E-MAIL: MARC. HERSHFI ELD@WI SCONSI N. GOV

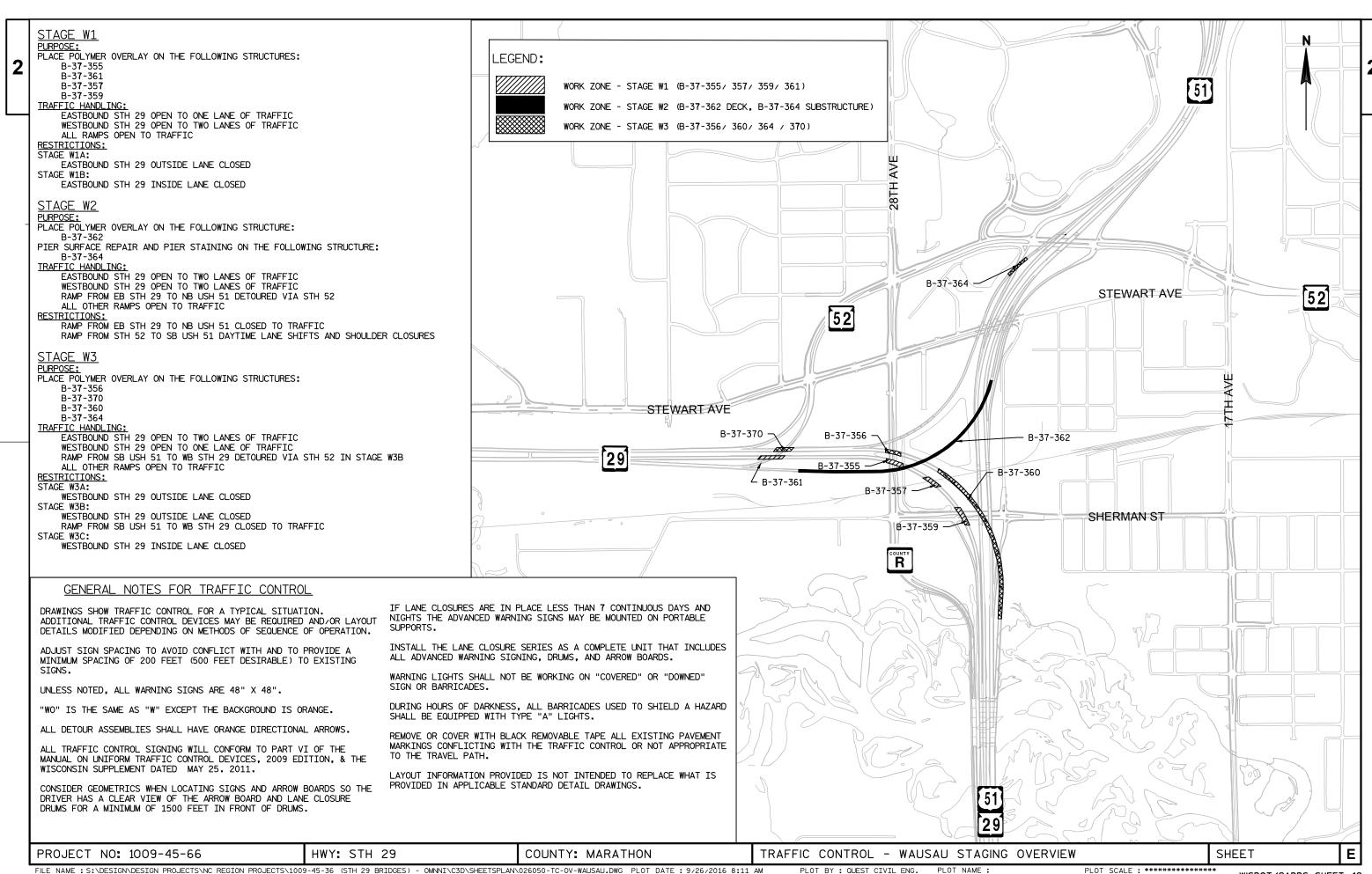
ORDER OF "SECTION 2" SHEETS

SHEET TITLE

GENERAL NOTES
PROJECT OVERVIEW
TRAFFIC CONTROL
DETOUR PLAN

PROJECT NO: 1009-45-66 HWY: STH 29 COUNTY: MARATHON GENERAL NOTES SHEET: E





PLACE POLYMER OVERLAY ON STRUCTURE B-37-337

TRAFFIC HANDLING:

RAMP FROM WB STH 29 TO IH 39 / USH 51 SB DETOURED VIA USH 51 NB TO CTH N ALL OTHER RAMPS REMAIN OPEN

RESTRICTIONS:

STAGE R1

RAMP FROM WB STH 29 TO IH 39 / USH 51 SB CLOSED

STAGE H1:

PURPOSE:

PLACE POLYMER OVERLAY ON STRUCTURE B-37-48 (INSIDE LANE) PLACE POLYMER OVERLAY ON STRUCTURE B-37-274 (INSIDE LANE) TRAFFIC HANDLING:

> EASTBOUND STH 29 OPEN TO ONE LANE OF TRAFFIC WESTBOUND STH 29 OPEN TO ONE LANE OF TRAFFIC ALL RAMPS OPEN TO TRAFFIC

RESTRICTIONS:

STAGE H1

EASTBOUND INSIDE LANE CLOSED WESTBOUND INSIDE LANE CLOSED

STAGE H2:

PURPOSE:

PLACE POLYMER OVERLAY ON STRUCTURE B-37-274 (OUTSIDE LANE) PLACE POLYMER OVERLAY ON STRUCTURE B-37-378

TRAFFIC HANDLING:

EASTBOUND STH 29 OPEN TO ONE LANE OF TRAFFIC WESTBOUND STH 29 OPEN TO TWO LANES OF TRAFFIC RAMP FROM EB STH 29 TO CTH Y DETOURED VIA U-TURN AT BASS LAKE ROAD ALL OTHER RAMPS OPEN TO TRAFFIC BASS LAKE ROAD CLOSED AT STH 29

RESTRICTIONS:

STAGE H2

EASTBOUND OUTSIDE LANE CLOSED RAMP FROM EB STH 29 TO CTH Y CLOSED TO TRAFFIC STAGES H2 AND H3 CANNOT OCCUR AT THE SAME TIME

STAGE H3:

PURPOSE:

PLACE POLYMER OVERLAY ON STRUCTURE B-37-48 (OUTSIDE LANE AND ON-RAMP) TRAFFIC HANDLING:

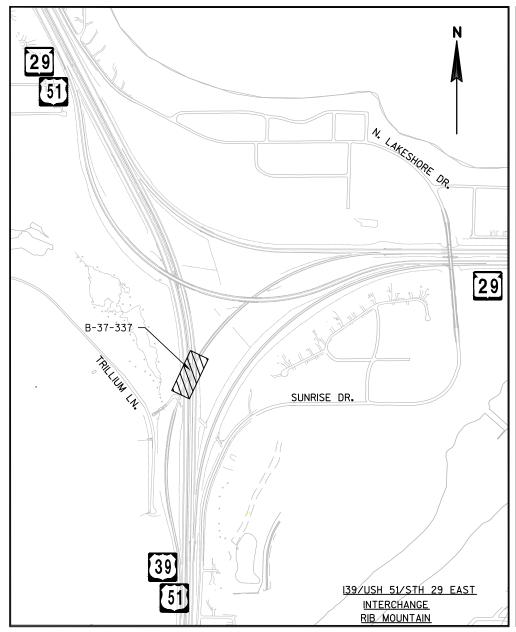
EASTBOUND STH 29 OPEN TO TWO LANES OF TRAFFIC WESTBOUND STH 29 OPEN TO ONE LANE OF TRAFFIC RAMP FROM CTH Y TO WB STH 29 DETOURED VIA STH 29 EB AND U-TURN AT BASS LAKE ROAD ALL OTHER RAMPS OPEN TO TRAFFIC BASS LAKE ROAD CLOSED AT STH 29

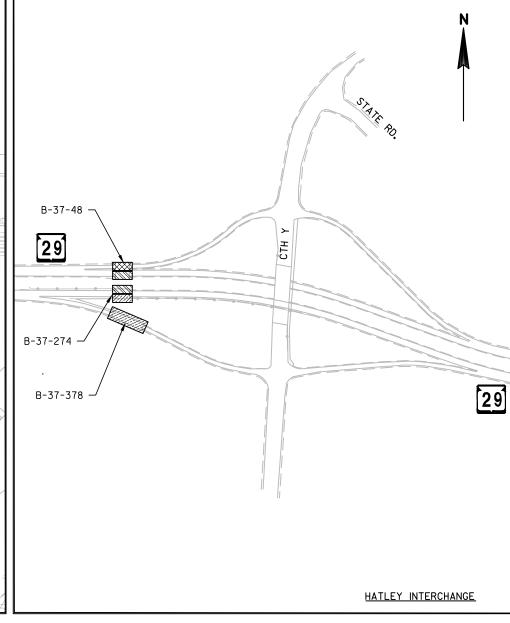
RESTRICTIONS:

PROJECT NO: 1009-45-66

STAGE H3

WESTBOUND OUTSIDE LANE CLOSED RAMP FROM CTH Y TO WB 29 CLOSED TO TRAFFIC STAGES H2 AND H3 CANNOT OCCUR AT THE SAME TIME





LEGEND

WORK ZONE - STAGE R1

WORK ZONE - STAGE H2

WORK ZONE - STAGE H1

WORK ZONE - STAGE H3

COUNTY: MARATHON

TRAFFIC CONTROL-RIB MOUNTAIN AND HATLEY STAGING OVERVIEW PLOT NAME :

HWY:STH 29

2

PORTABLE CHANGEABLE MESSAGE SIGNS

2

WAUSAU

				PRIOR TO CONSTRUCTION		STAGE W1		STAGE W2		STAGE W3A		STAGE W3B	
STRUCTURE			ROADWAY CONDITION/	PHASE I	PHASE II	PHASE I	PHASE II	PHASE I	PHASE II	PHASE I	PHASE II	PHASE I	PHASE II
NO	SITE NO	LOCATION	CONSTRUCTION	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)
WAUSAU AREA B-37-355	W-EB-1	ON EB STH 29 EAST OF 72ND AVE	GENERAL	STH 29 ROAD WORK	BEGINS XX/XX/XX	(NO ME	O MESSAGE) (NO MESSAGE)		ESSAGE)	(NO MESSAGE)		(NO MESSAGE)	
B-37-356 B-37-357	W-EB-2	ON EB STH 29 AUX LANE FOR OFF RAMP TO STH 52/USH 51	GENERAL	(NO MESSAGE)		NORTH 51 RAMP TO CLOSE	xx/xx/xx	NORTH 51 RAMP CLOSED	DETOUR USE HWY 52	(NO MESSAGE)		(NO MESSAGE)	
B-37-359 B-37-360 B-37-361 B-37-362	W-NB-1	ON NB USH 51/STH 29 AT CTH NN	GENERAL	(NO MESSAGE)		(NO MESSAGE)		STH 29 ROAD WORK	BEGINS XX/XX/XX	(NO MESSAGE)		(NO MESSAGE)	
B-37-362 B-37-364 B-37-370	W-SB-1	ON SB USH 51/STH 29 AT BRIDGE STREET	GENERAL	(NO ME	(NO MESSAGE)		(NO MESSAGE)		(NO MESSAGE)		xx/xx/xx	WEST 29 RAMP CLOSED	DETOUR EXIT 192

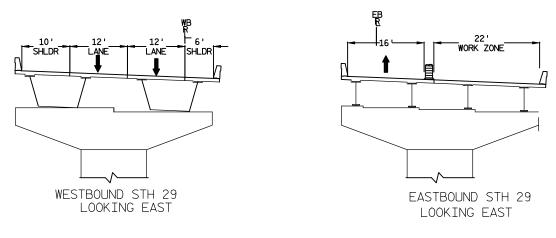
RIB MOUNTAIN

				PRIOR TO CO	ONSTRUCTION	STAGE R1		
STRUCTURI	<u>:</u>		ROADWAY CONDITION/	FRAME I	FRAME II	FRAME I	FRAME II	
NO	SITE NO	LOCATION	CONSTRUCTION	(2 SEC)	(2 SEC)	(2 SEC)	(2 SEC)	
RIB		ONE HALF MILE EAST OF		STH 29	BEGINS	IH 39 SB	FOLLOW	
MOUNTAIN	MOUNTAIN WB 1	PROJECT SITE	GENERAL	ROAD	XX/XX/XX	RAMP	DETOUR	
в-37-337				WORK	^^/ ^^/ ^^	CLOSED		

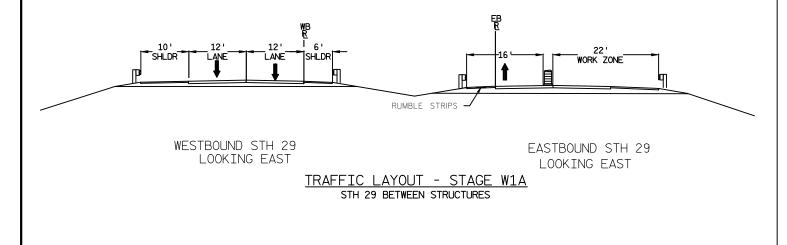
HATLEY

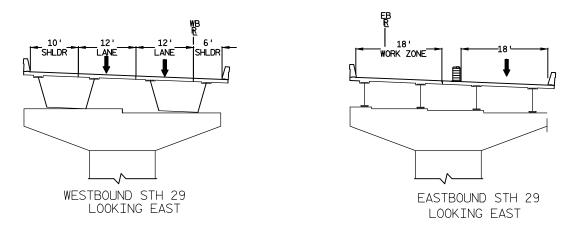
				PRIOR TO CONSTRUCTION		STAGE H1		STAGE H2		STAGE H3	
STRUCTURE NO	SITE NO	LOCATION	ROADWAY CONDITION/ CONSTRUCTION	PHASE I (2 SEC)	PHASE II (2 SEC)	PHASE I (2 SEC)	PHASE II (2 SEC)	PHASE I (2 SEC)	PHASE II (2 SEC)	PHASE I (2 SEC)	PHASE II (2 SEC)
HATLEY B-37-274 B-37-378 B-37-48	EB 1	ON EB STH 29 WEST OF CTH Y	GENERAL	STH 29 ROAD WORK	BEGINS XX/XX/XX	RAMP TO CLOSE	MP XX/XX/XX (NO		SSAGE)	(NO MESSAGE)	
	EB 2	ON EB STH 29 EAST OF CTH Y	GENERAL	(NO MESSAGE)		(NO MESSAGE)		TRAFFIC EXITING AHEAD	BE ALERT	TRAFFIC EXITING AHEAD	BE ALERT
	WB 1	ON WB STH 29 EAST OF BASS LAKE ROAD	GENERAL	STH 29 ROAD WORK	BEGINS XX/XX/XX	(NO ME	ESSAGE)	TRAFFIC ENTERING AHEAD	BE ALERT	TRAFFIC ENTERING AHEAD	BE ALERT
	WB 2	ON WB STH 29 EAST OF HILLTOP ROAD	GENERAL	(NO MESSAGE)		(NO MESSAGE)		BASS LAKE ROAD CLOSED	USE ALT ROUTE	BASS LAKE ROAD CLOSED	USE ALT ROUTE
	NB 1	ONE MILE SOUTH OF PROJECT SITE	GENERAL	STH 29 ROAD WORK	BEGINS XX/XX/XX	(NO MESSAGE)		(NO MESSAGE)		RAMP TO CLOSE	BEGINS XX/XX/XX
	SB 1	ONE MILE NORTH OF PROJECT SITE	GENERAL	STH 29 ROAD WORK	BEGINS XX/XX/XX	(NO MESSAGE) (NO MESSAGE)		SSAGE)	RAMP TO CLOSE	BEGINS XX/XX/XX	

PROJECT NO: 1009-45-66 HWY: STH 29 COUNTY: MARATHON TRAFFIC CONTROL – PCMS MESSAGES SHEET: E

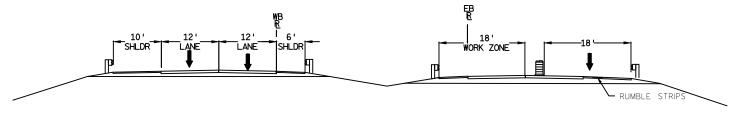


TRAFFIC LAYOUT - STAGE W1A STH 29 ON STRUCTURE





TRAFFIC LAYOUT - STAGE W1B STH 29 ON STRUCTURE



WESTBOUND STH 29 LOOKING EAST

EASTBOUND STH 29 LOOKING EAST

TRAFFIC LAYOUT - STAGE W1B STH 29 BETWEEN STRUCTURES

PROJECT NO:1009-45-66

HWY:STH 29

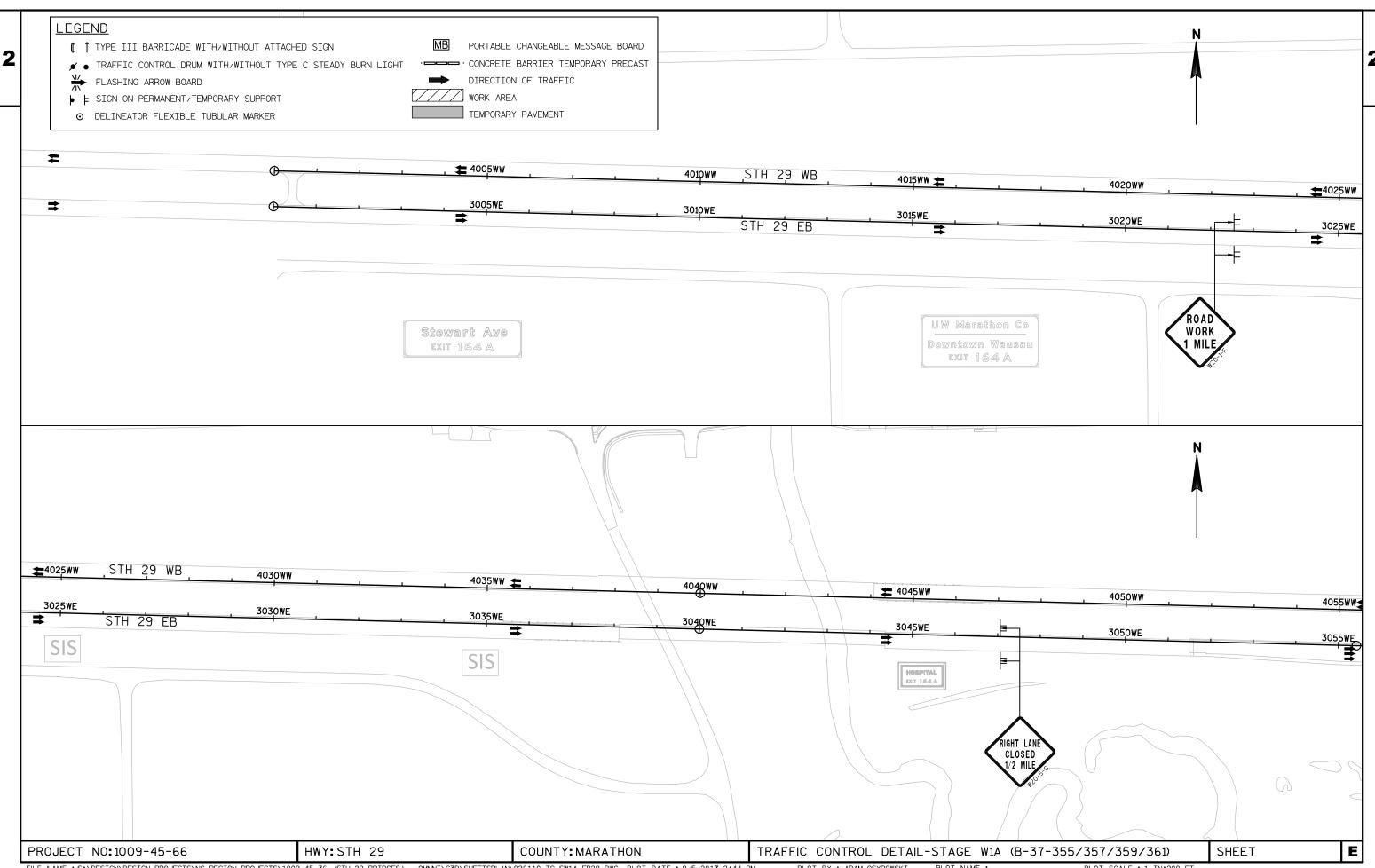
COUNTY: MARATHON

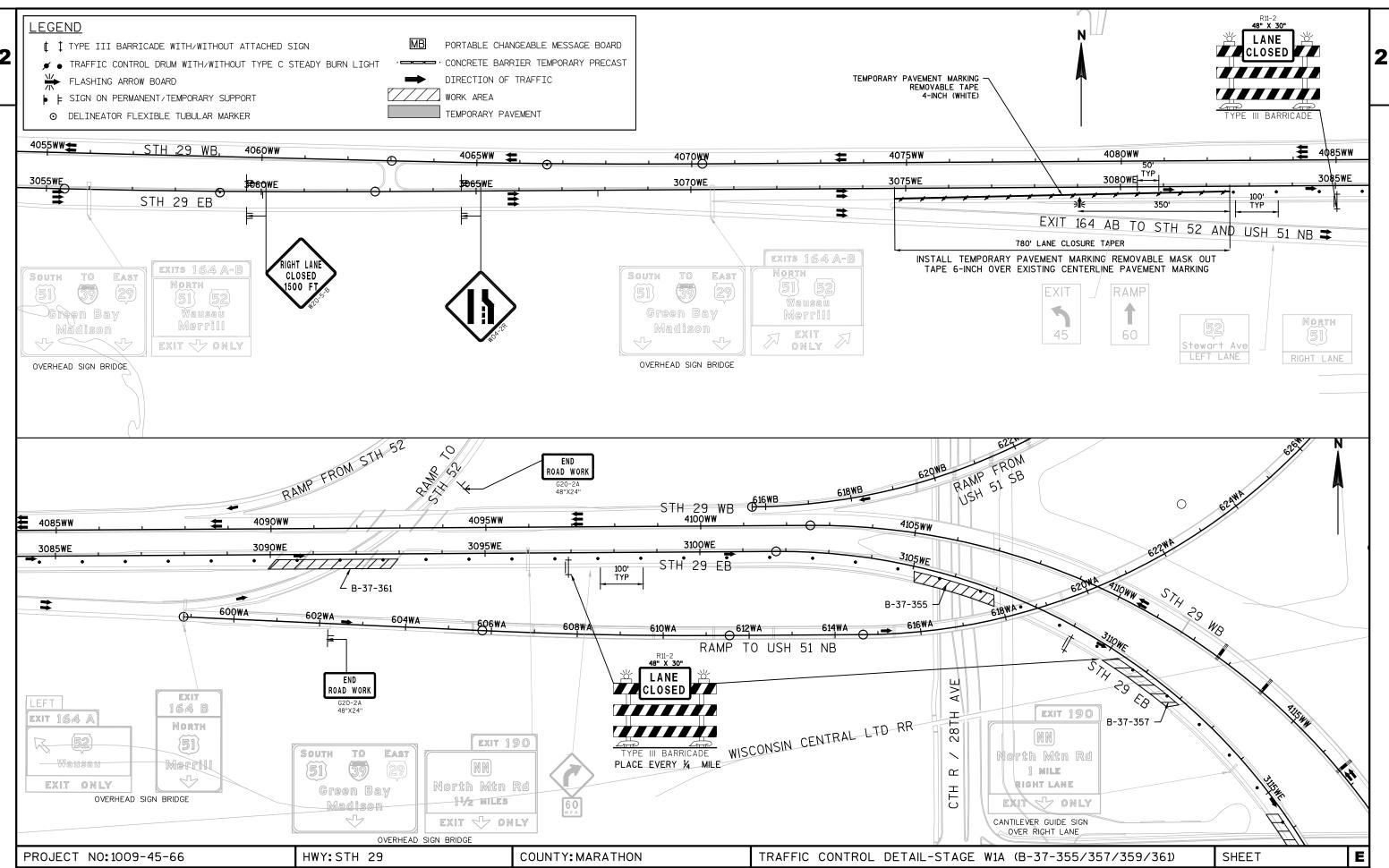
TRAFFIC CONTROL TYPICAL - STAGE W1 (B-37-355/357/359/361)

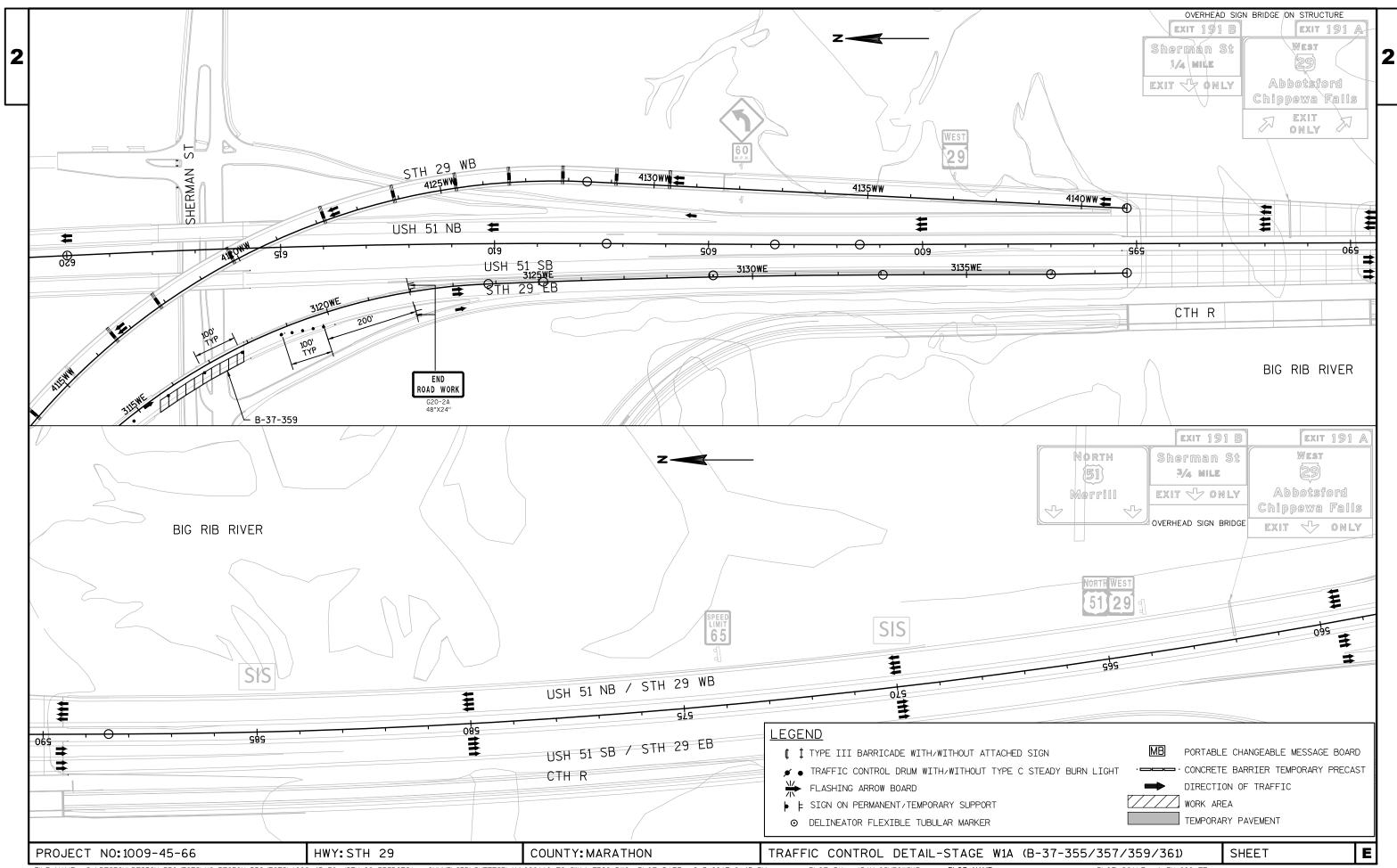
PLOT SCALE : 1 IN:20 FT

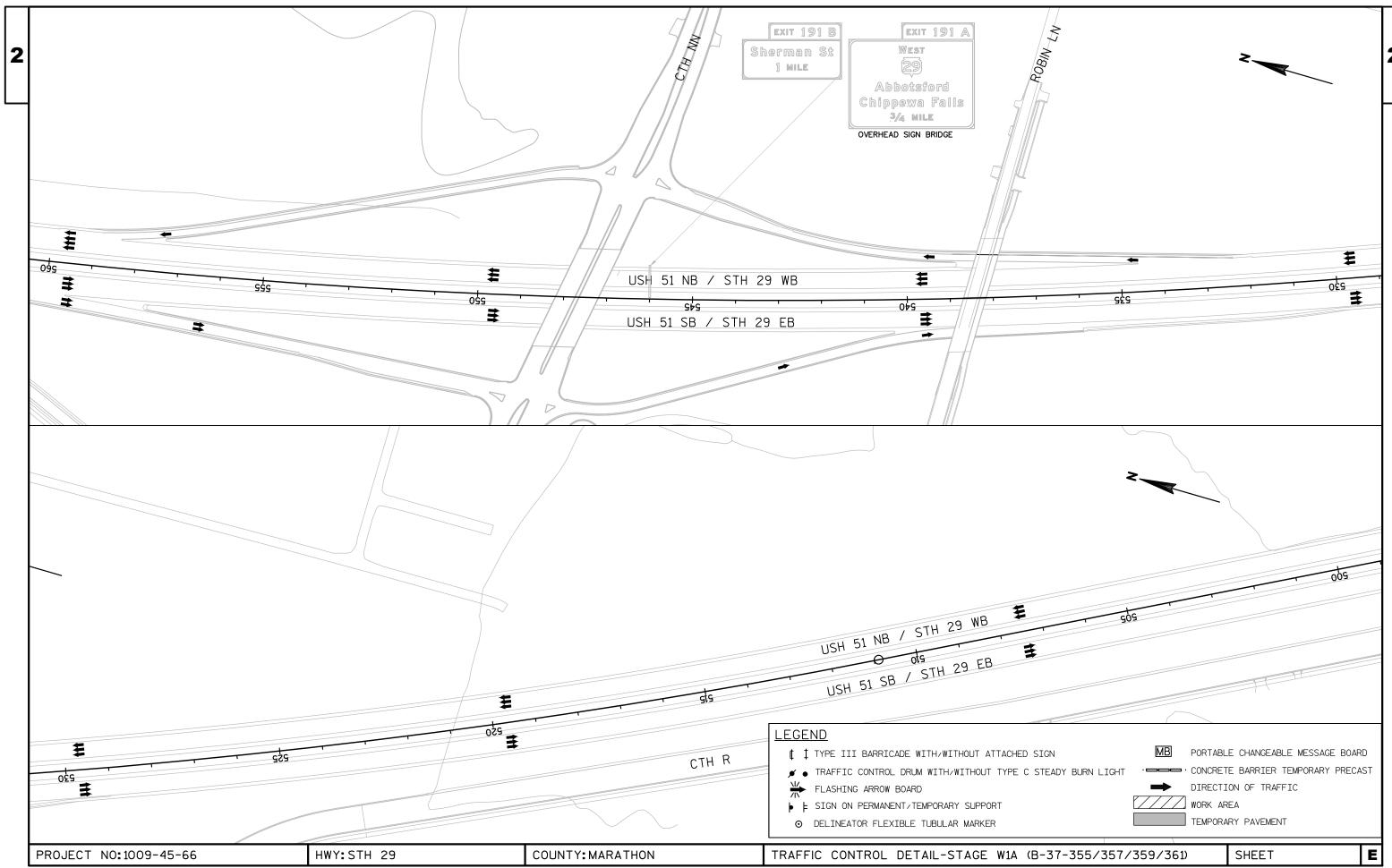
SHEET

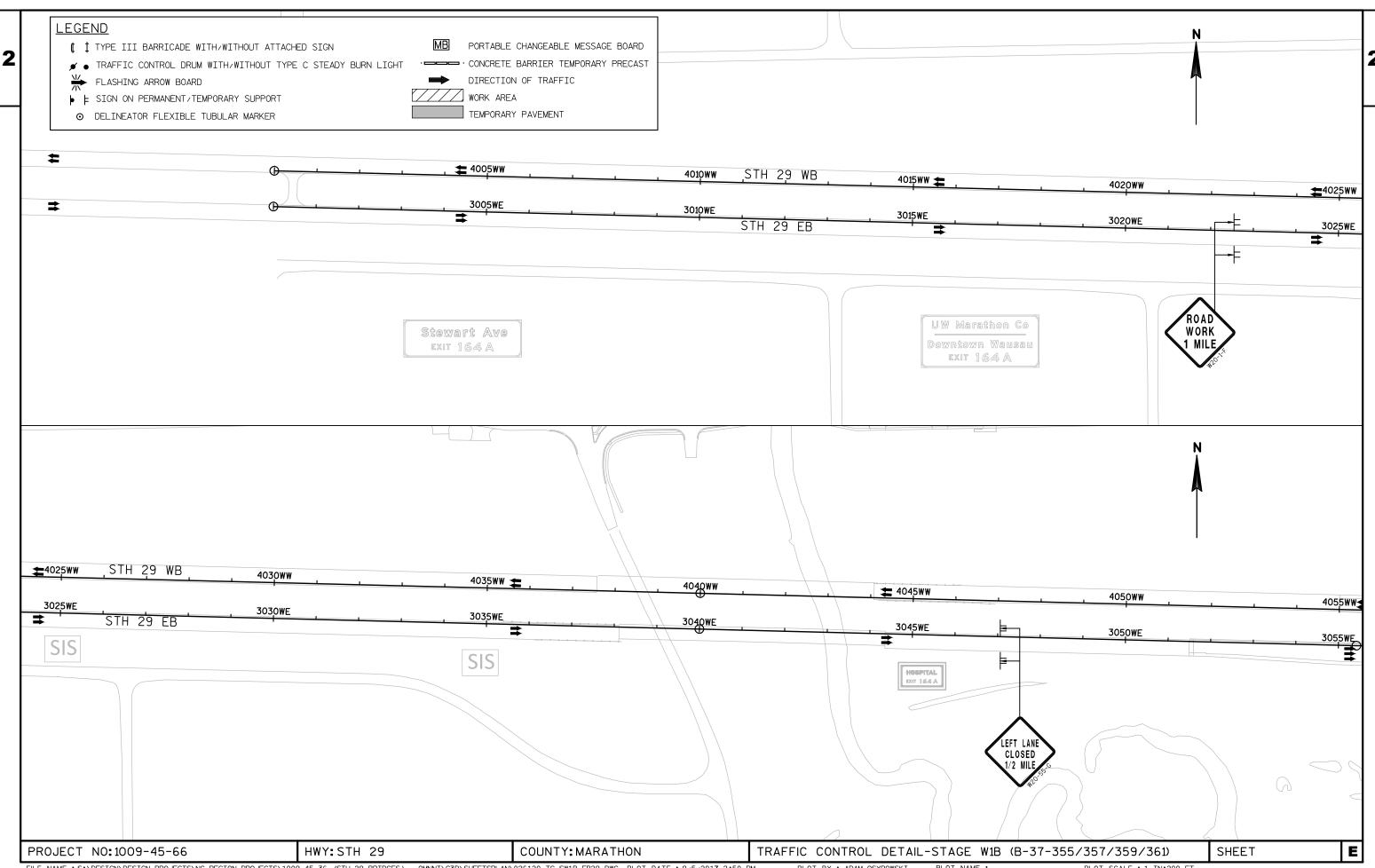
E

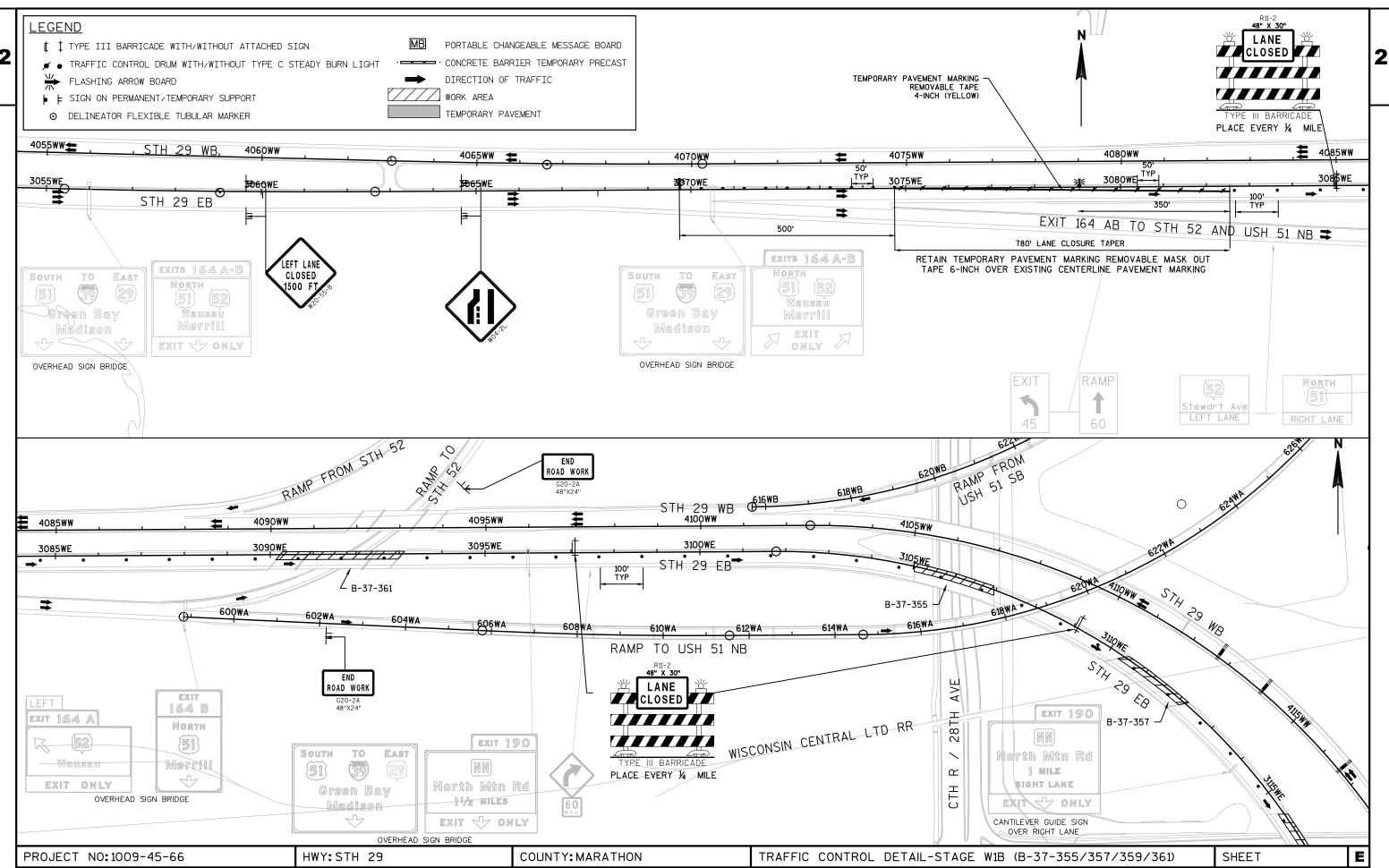


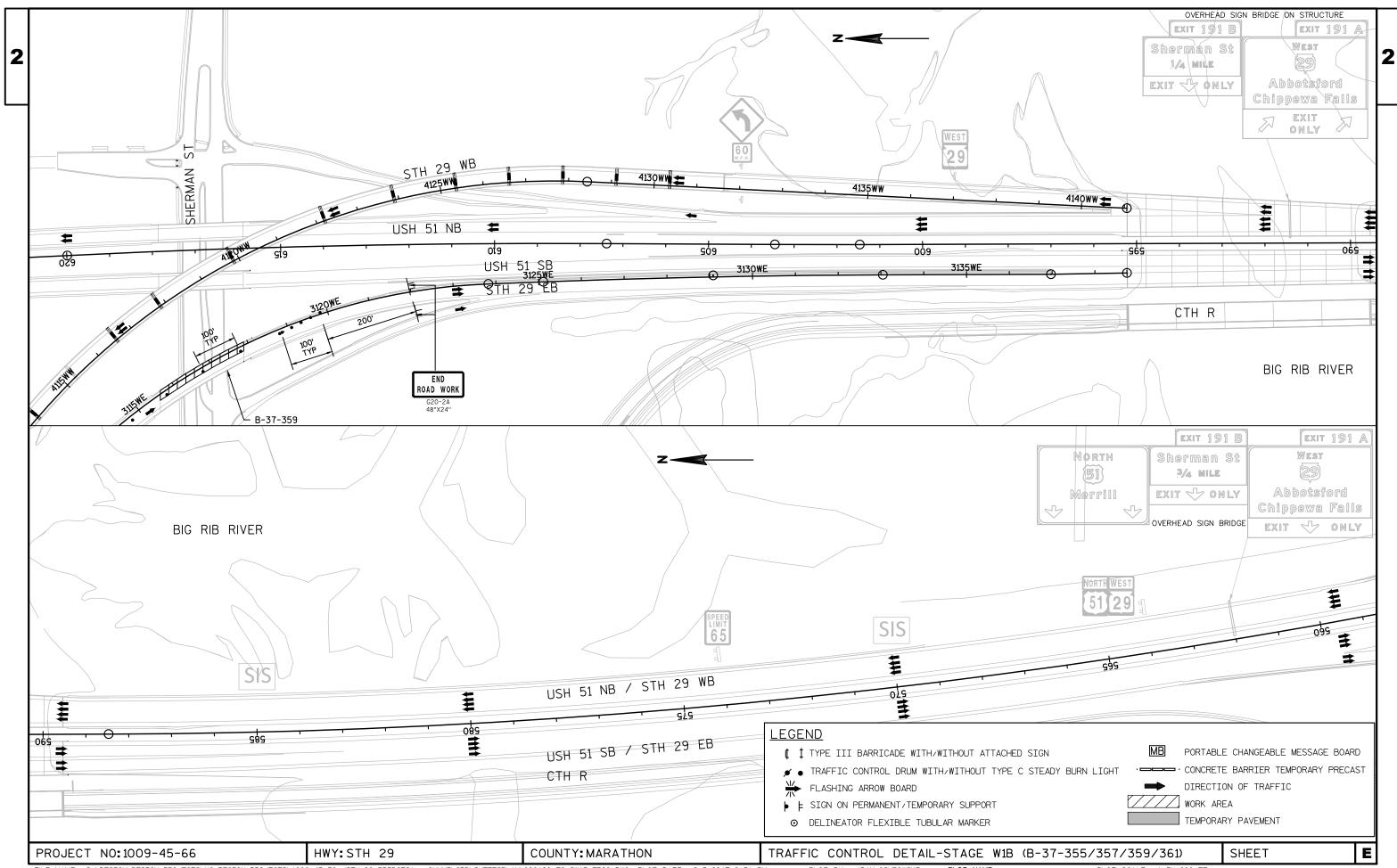


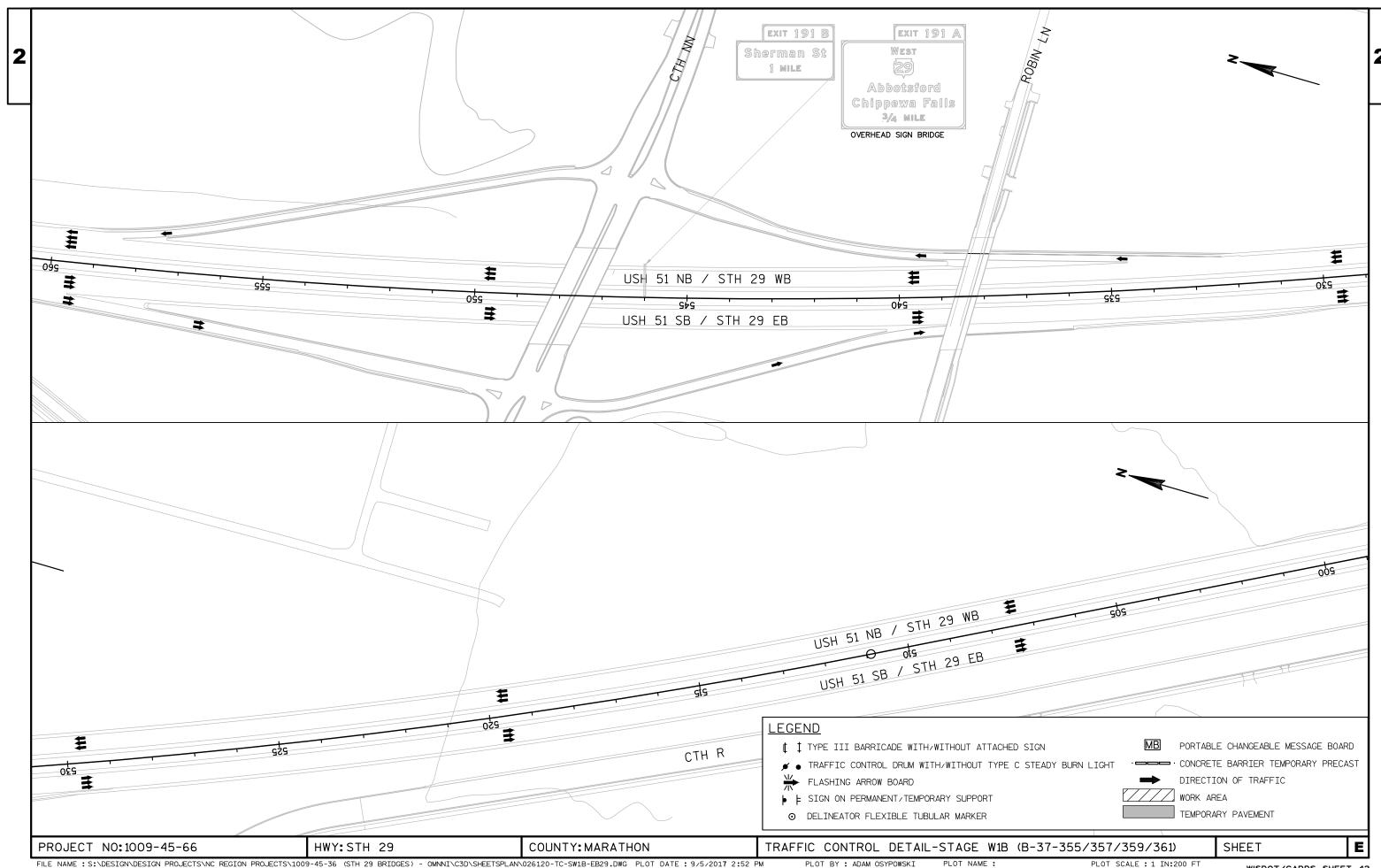


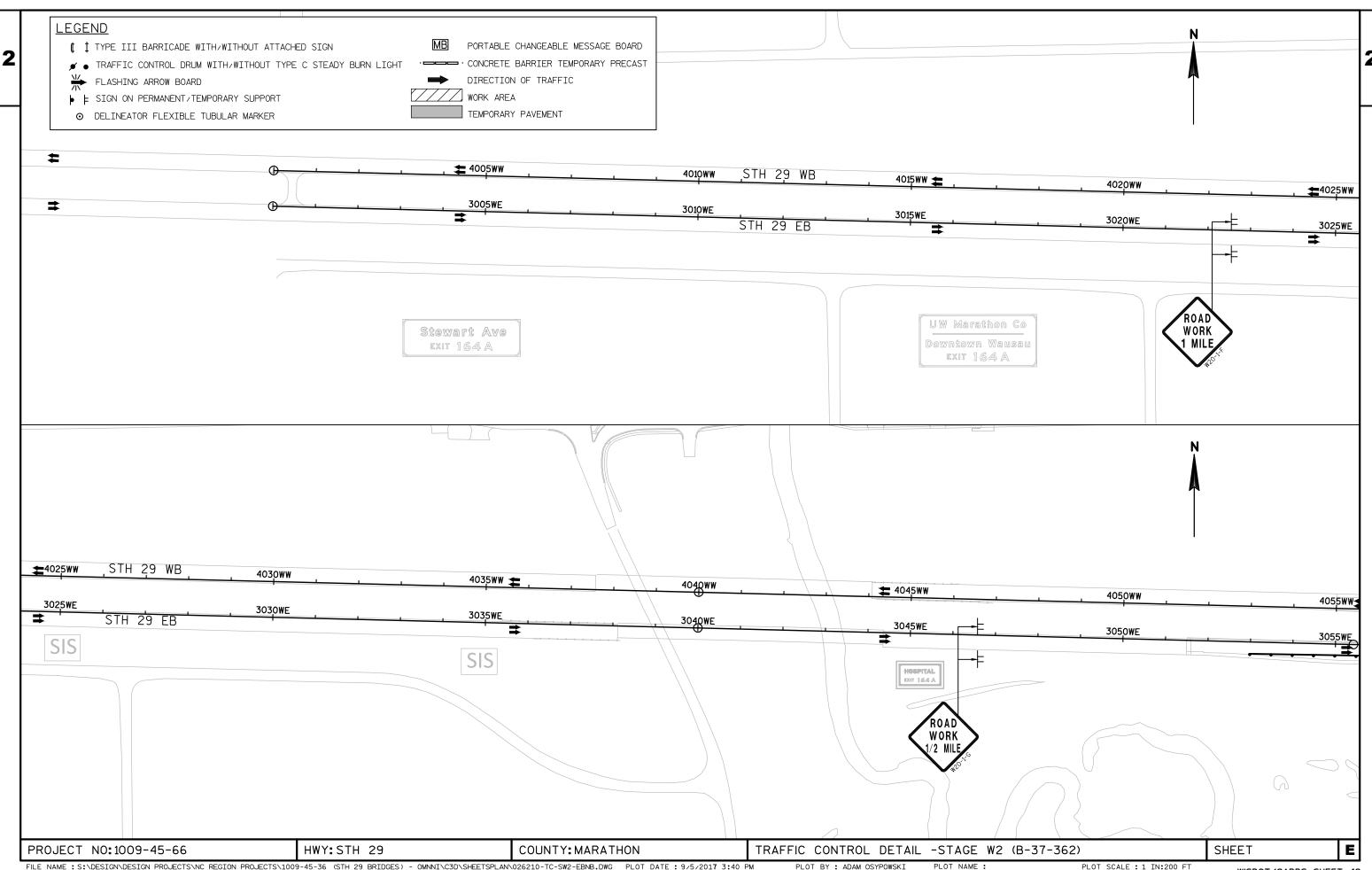


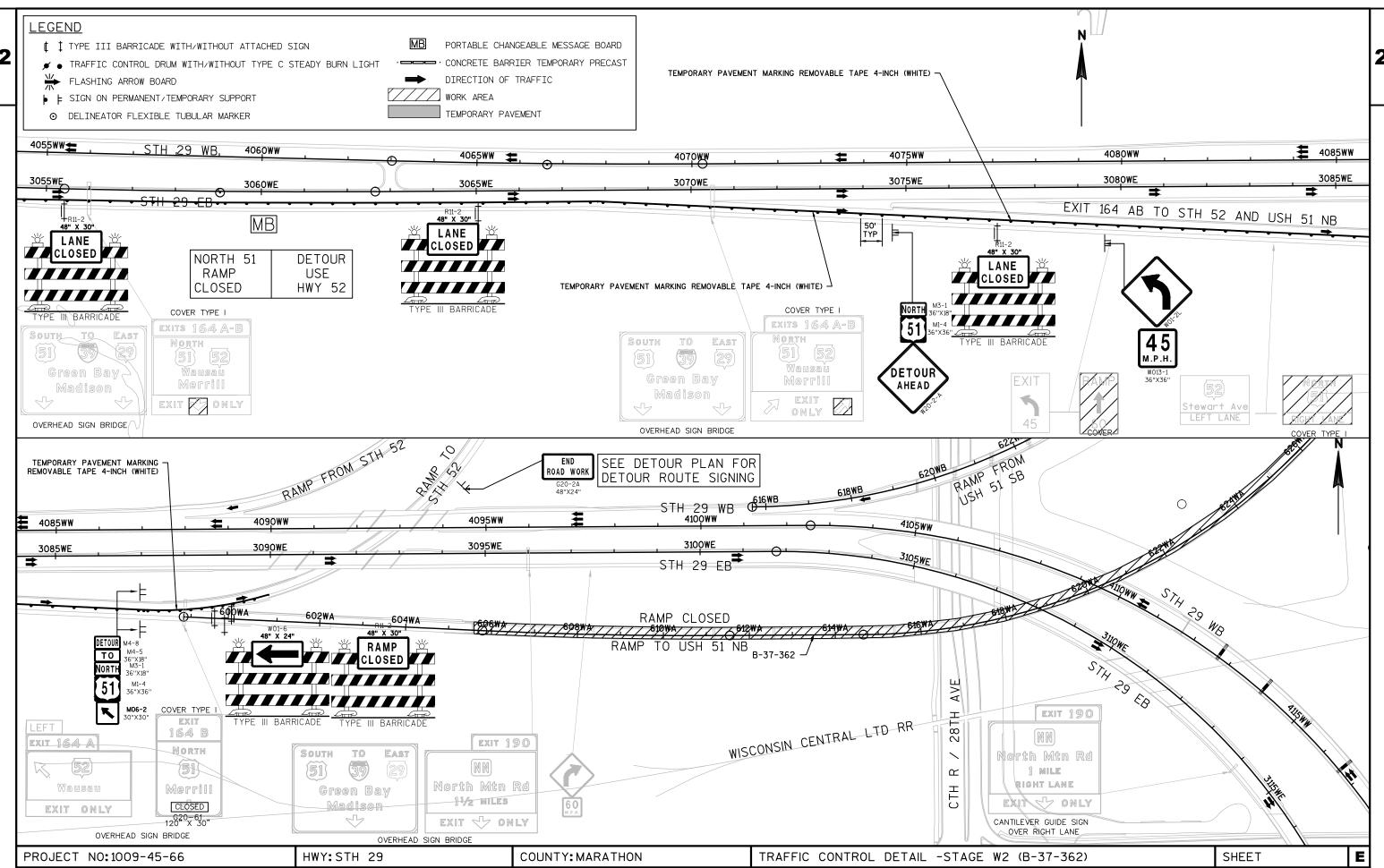


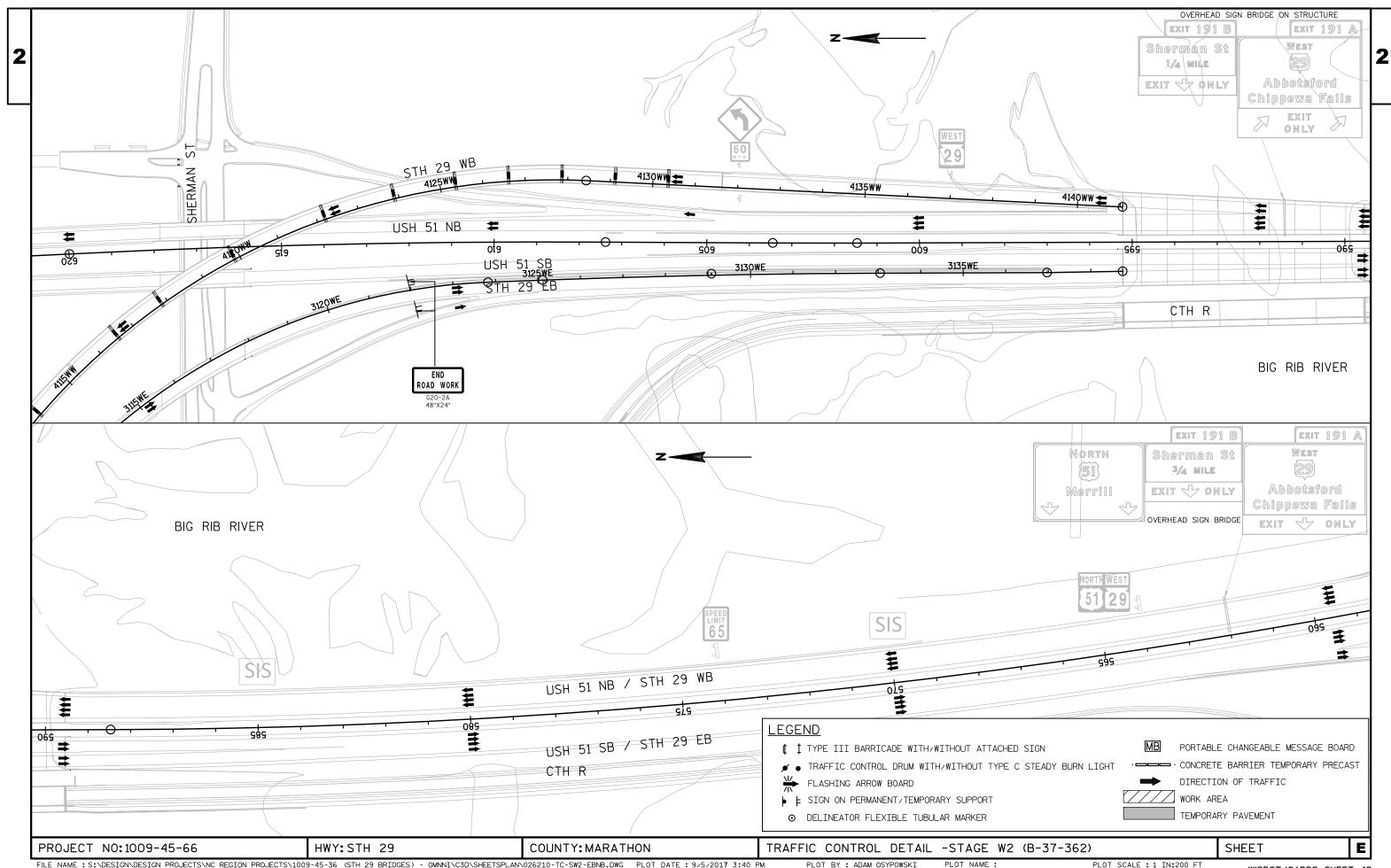


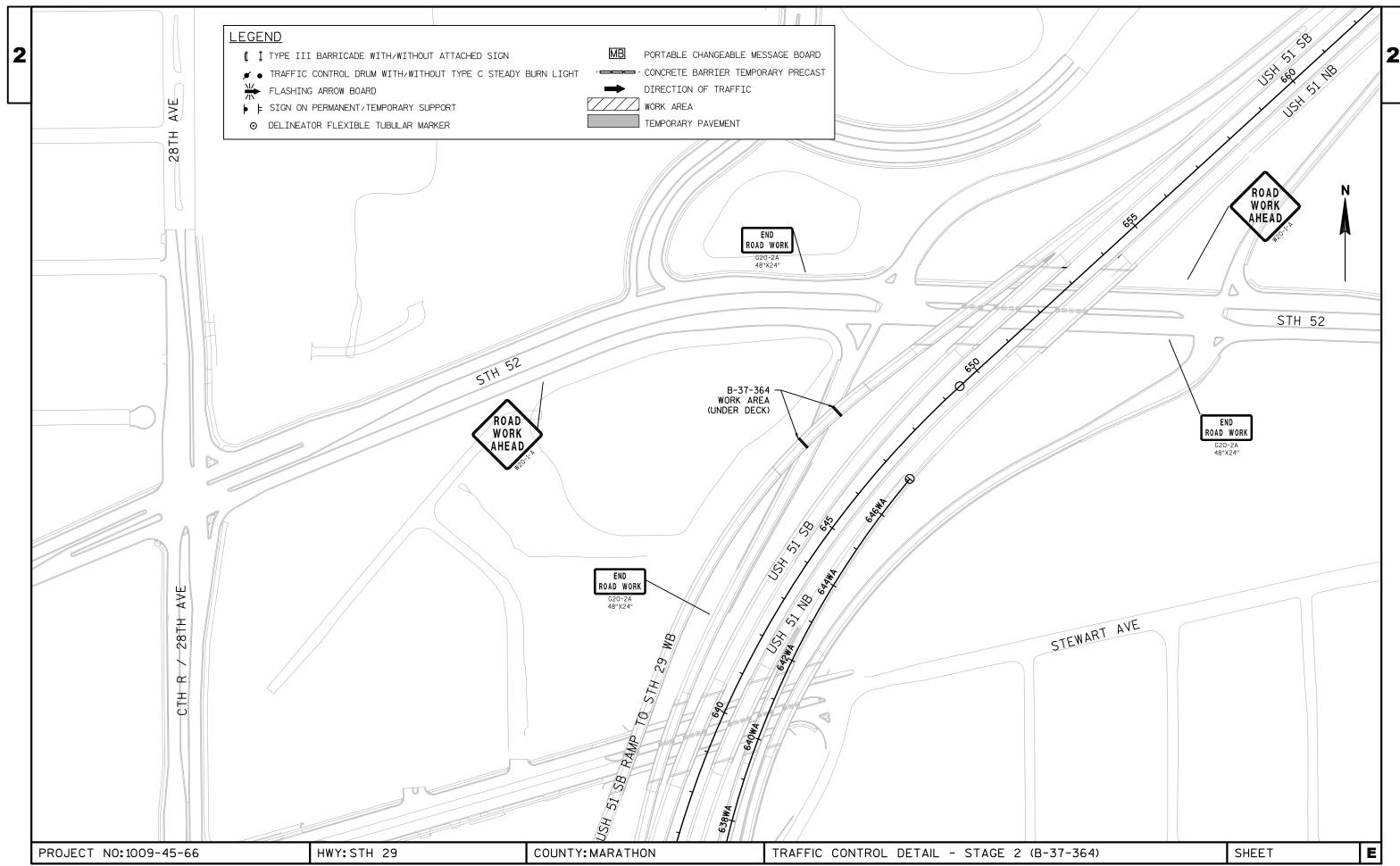


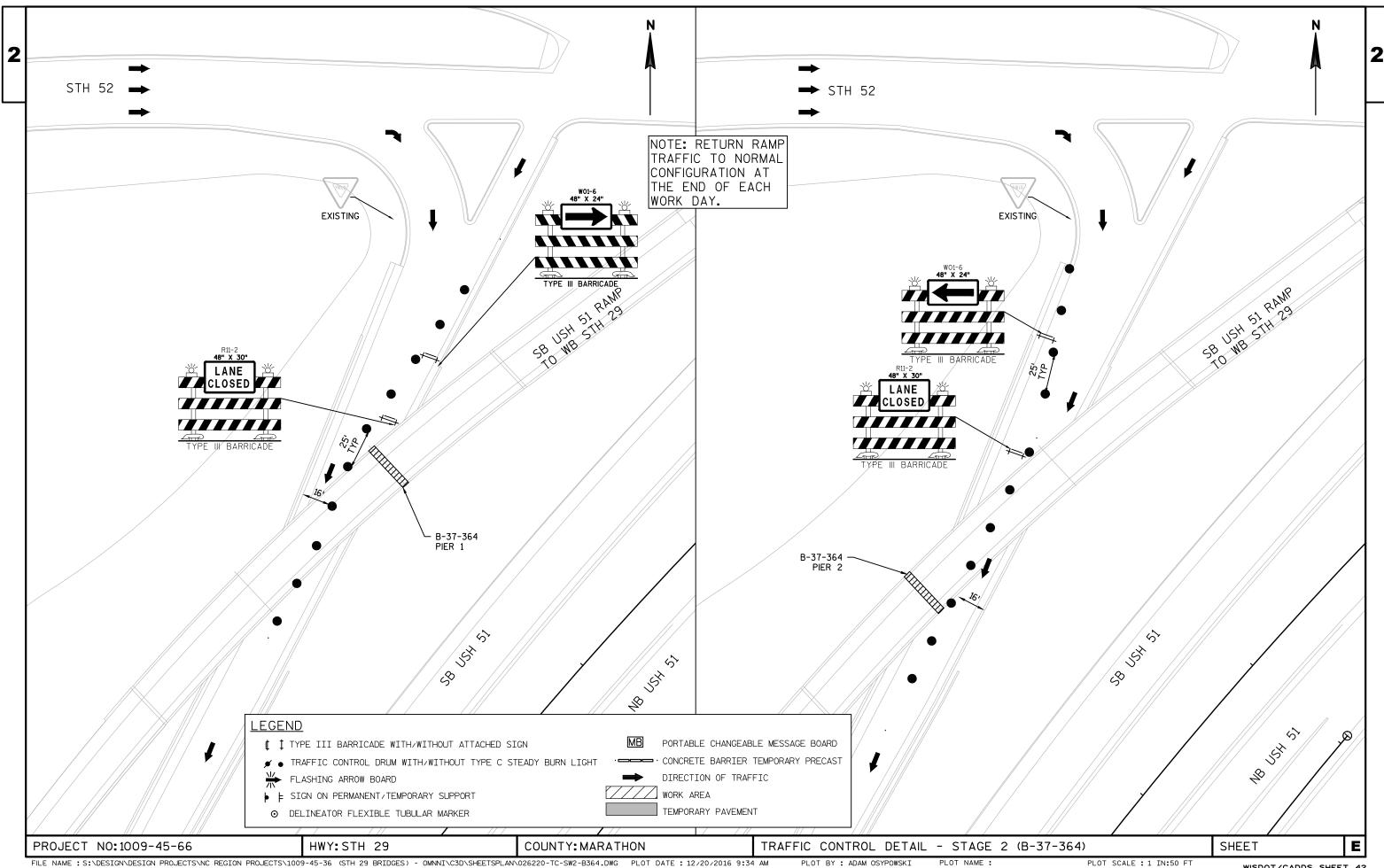


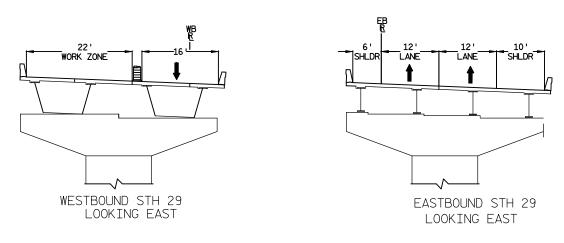




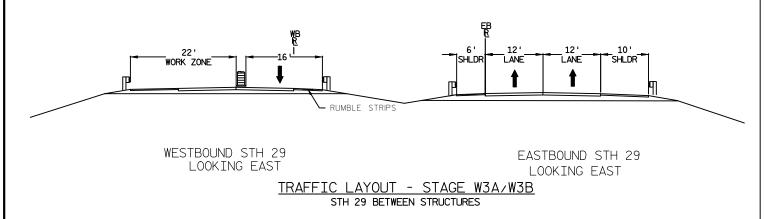


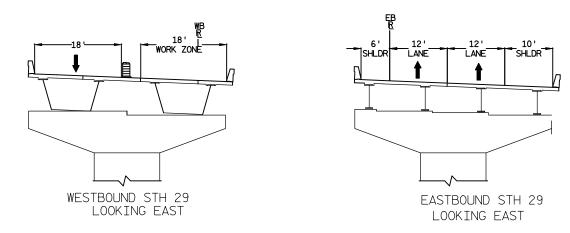




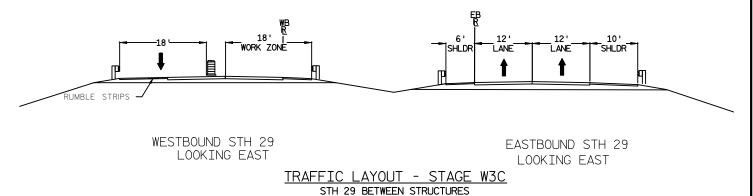


TRAFFIC LAYOUT - STAGE W3A/W3B STH 29 ON STRUCTURE

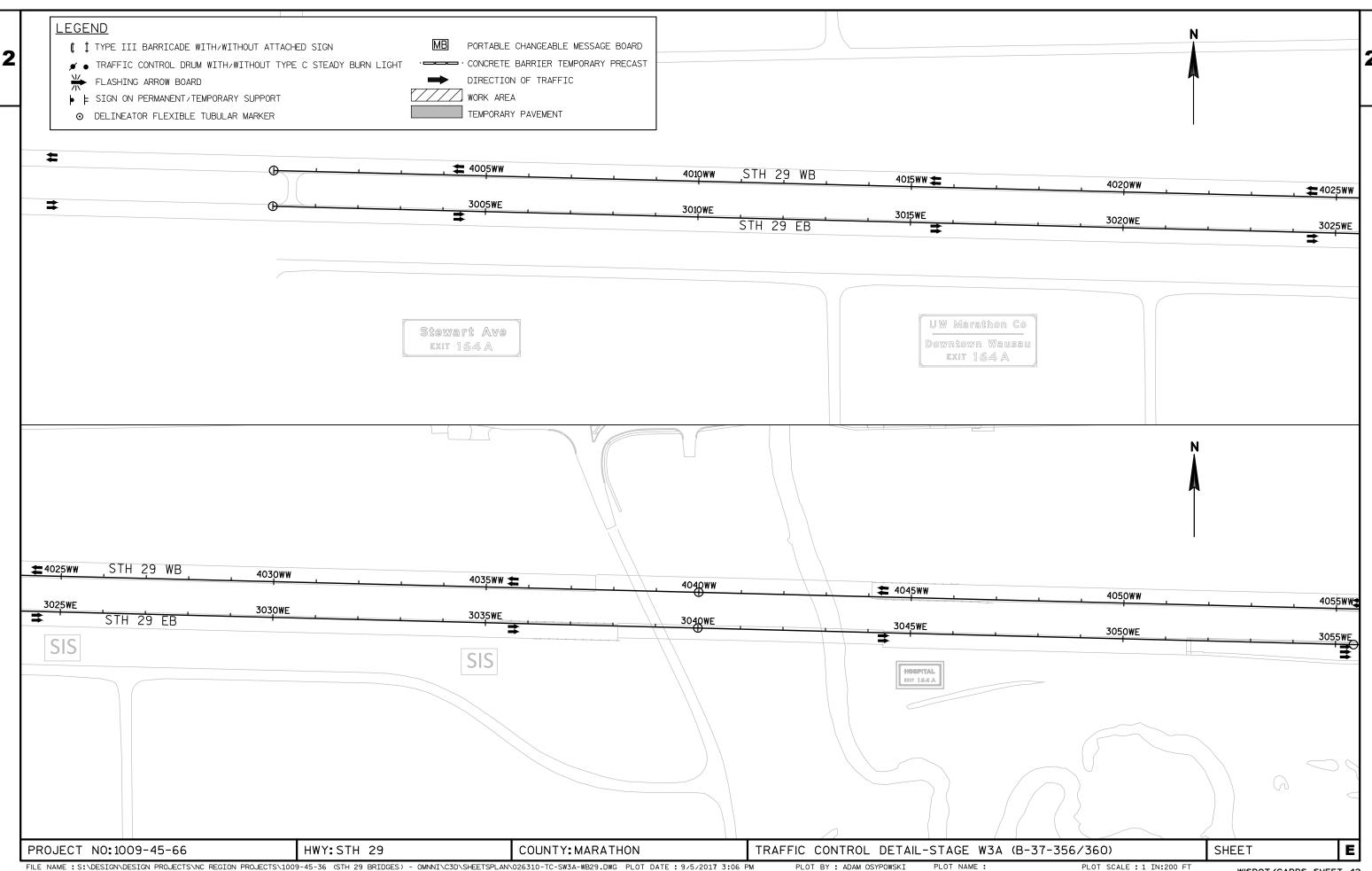


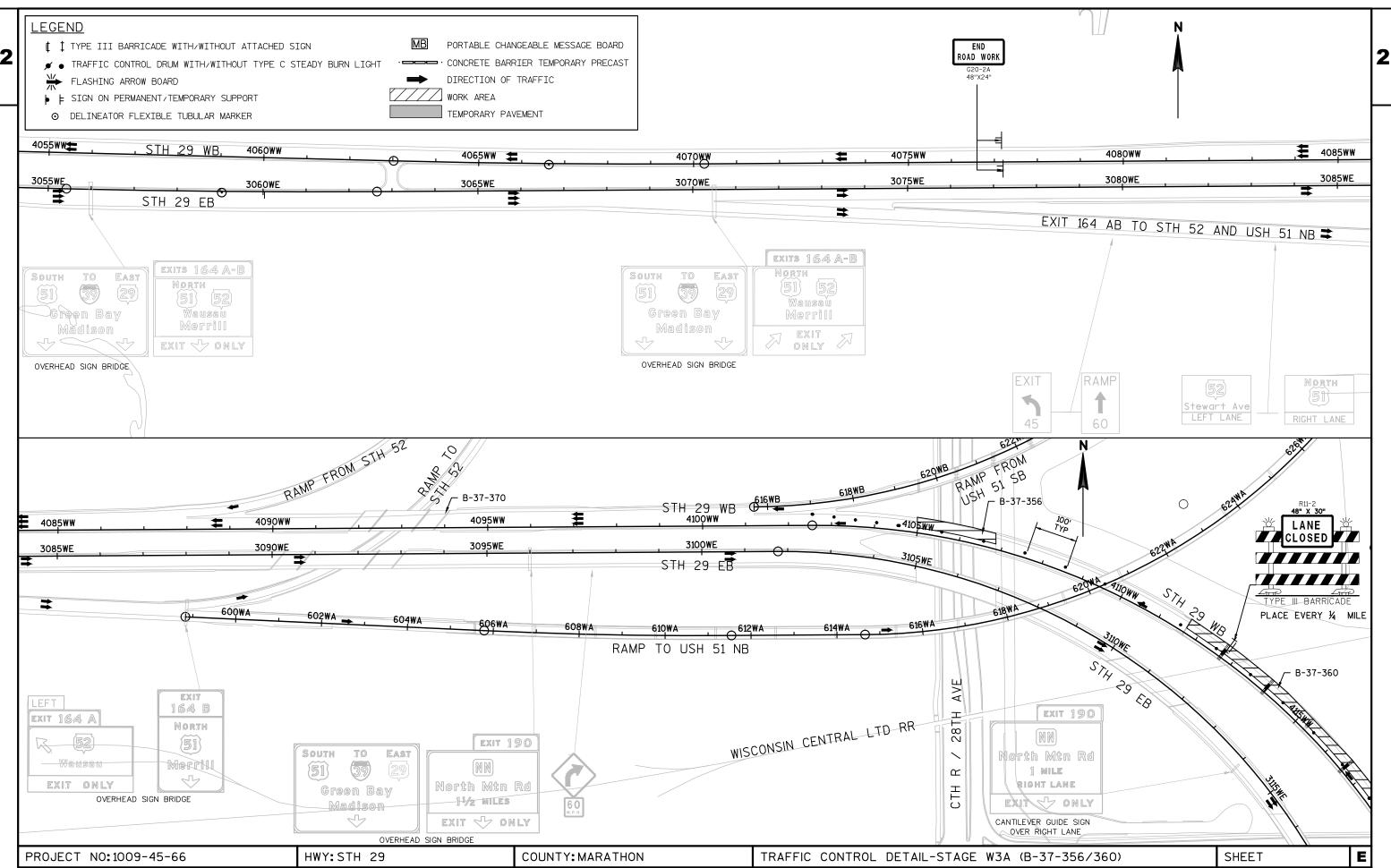


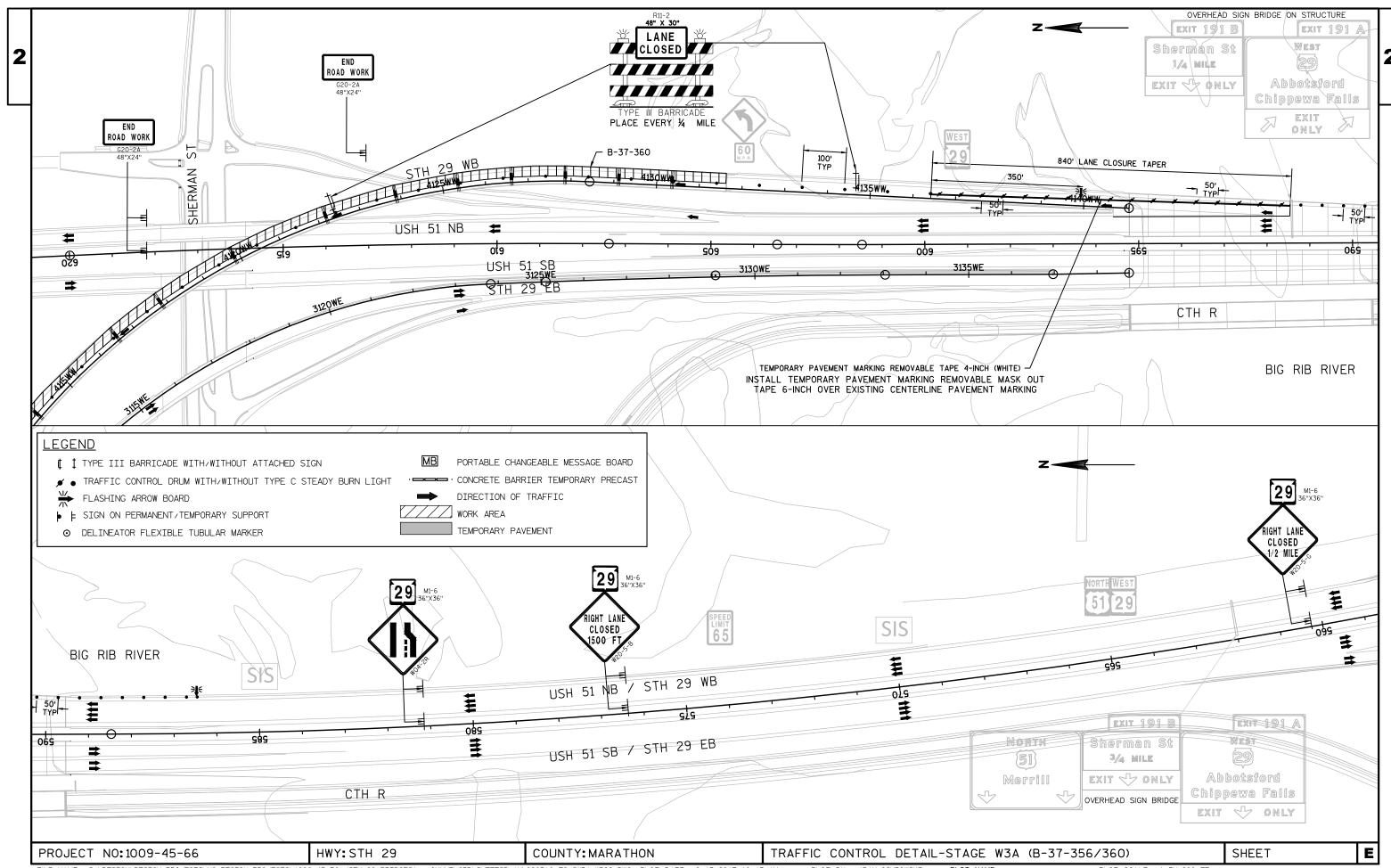
TRAFFIC LAYOUT - STAGE W3C
STH 29 ON STRUCTURE

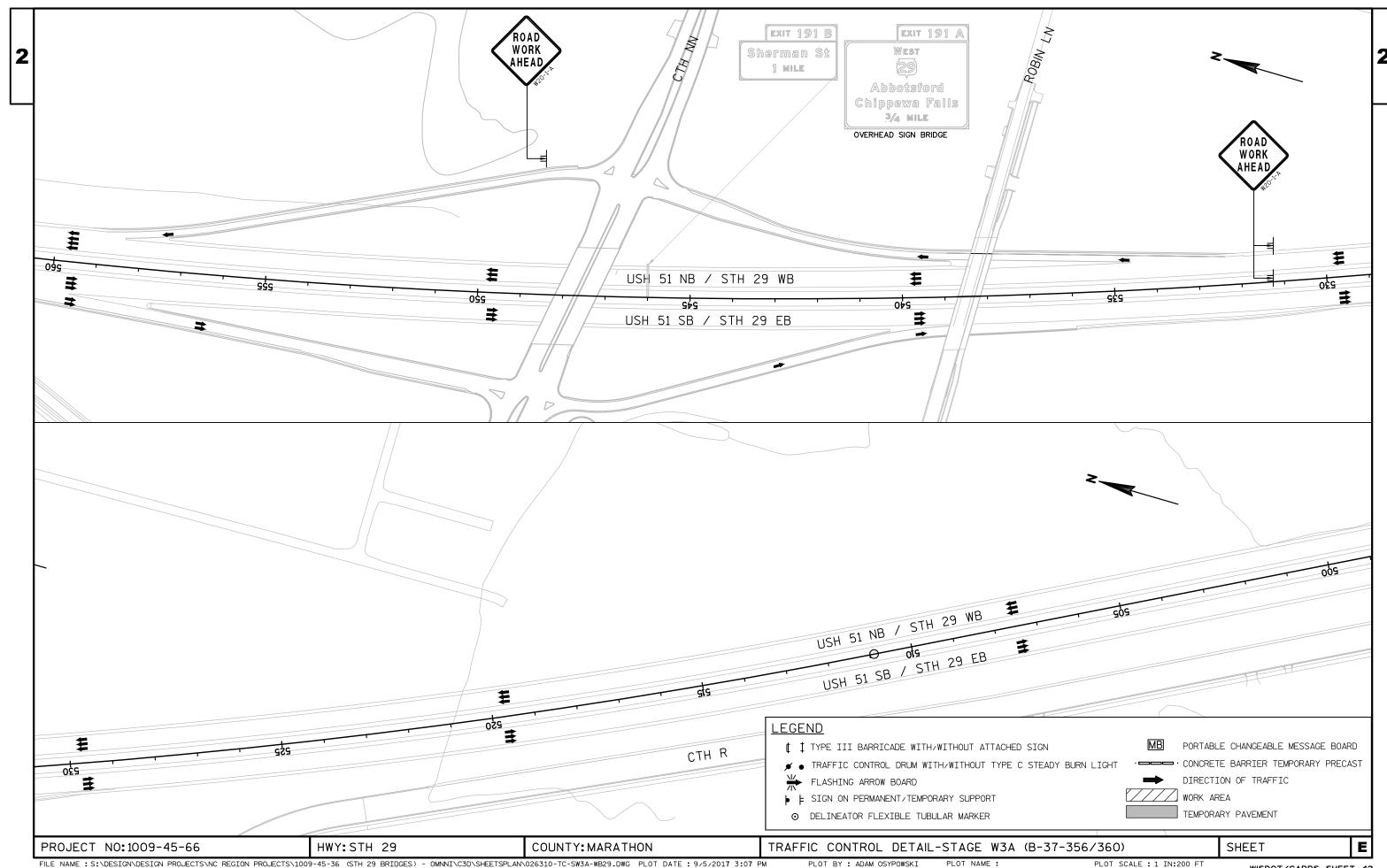


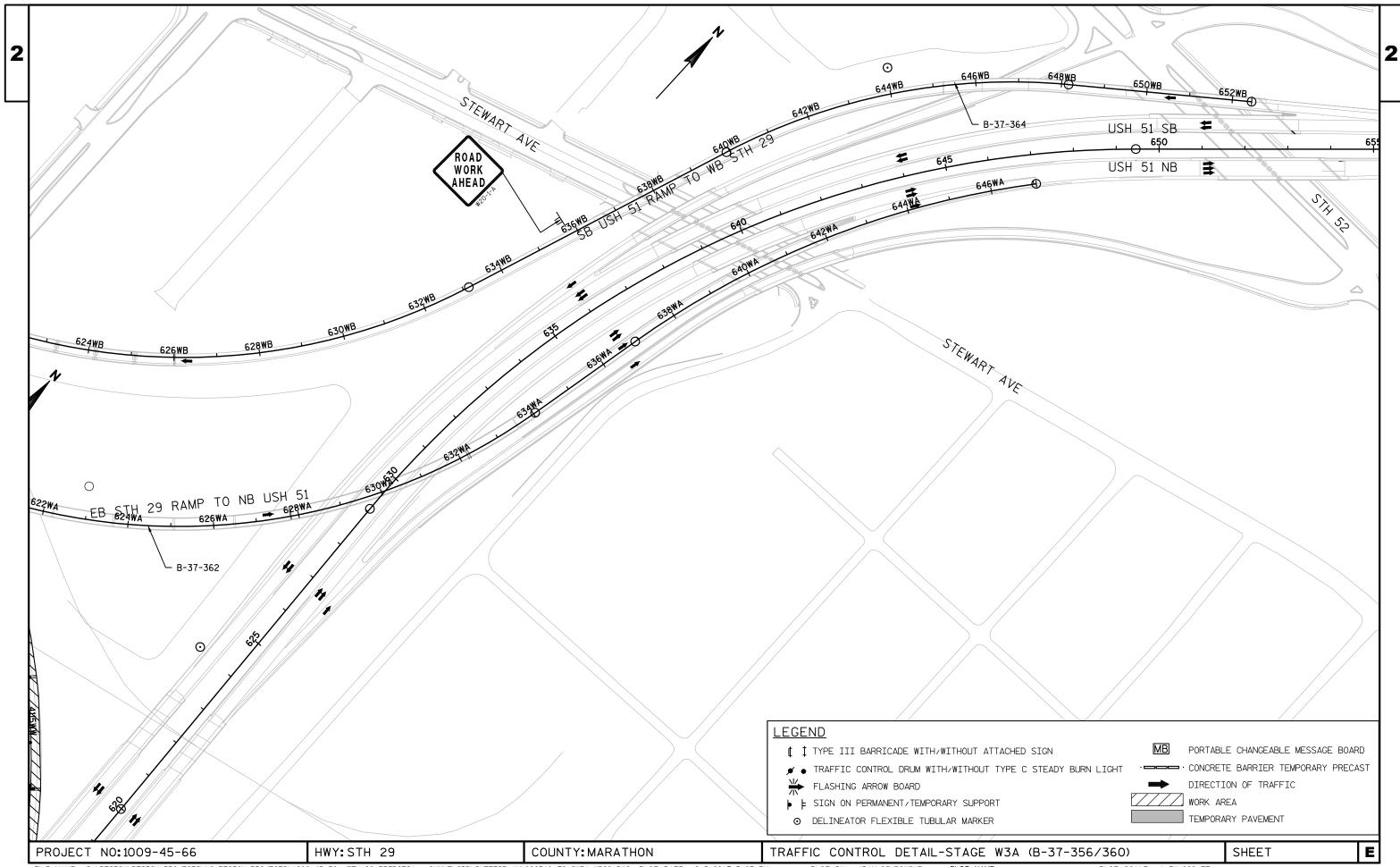
PROJECT NO:1009-45-66 HWY:STH 29 COUNTY:MARATHON TRAFFIC CONTROL TYPICAL - STAGE W3 (B-37-356/360/364/370) SHEET

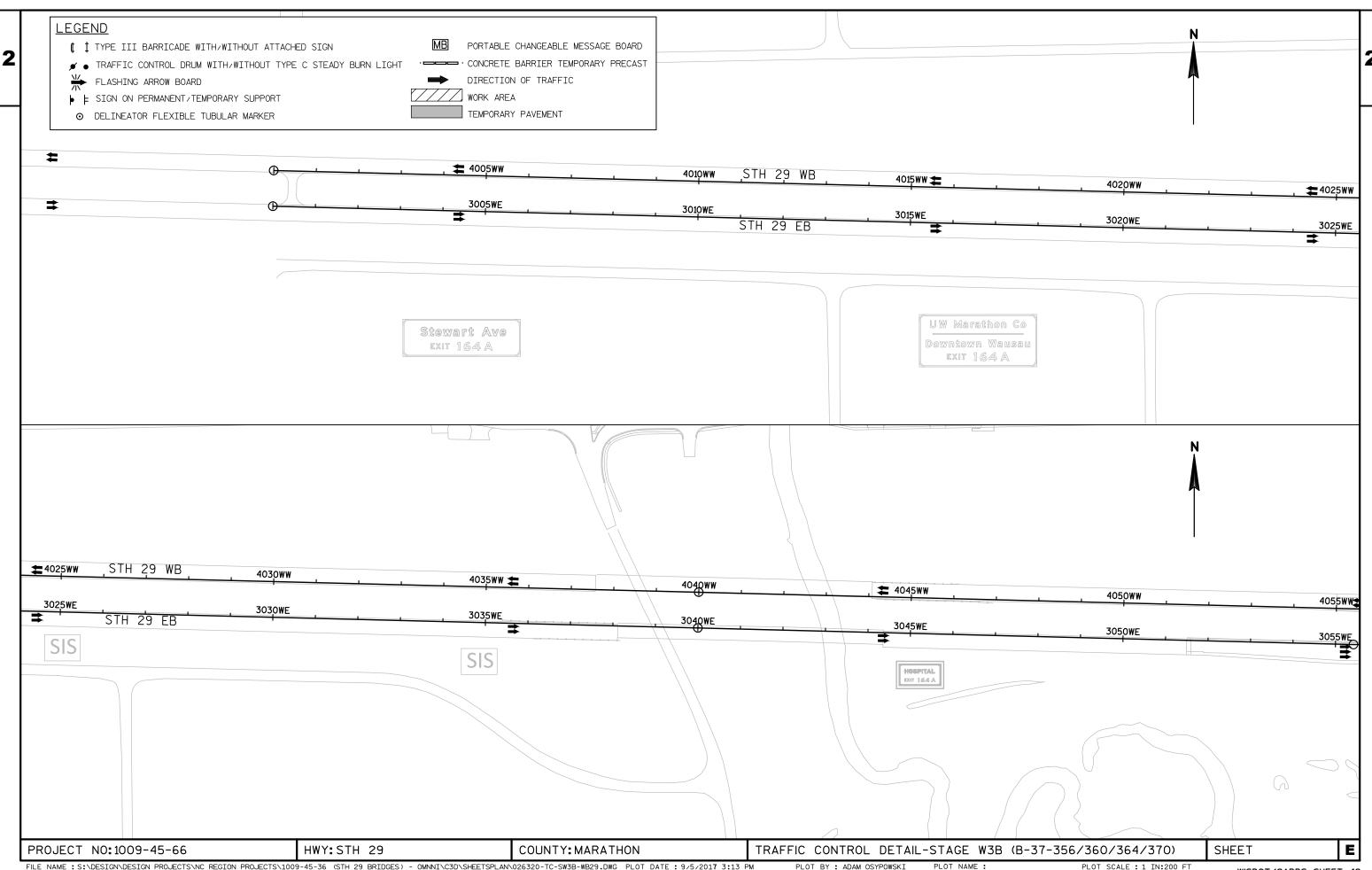


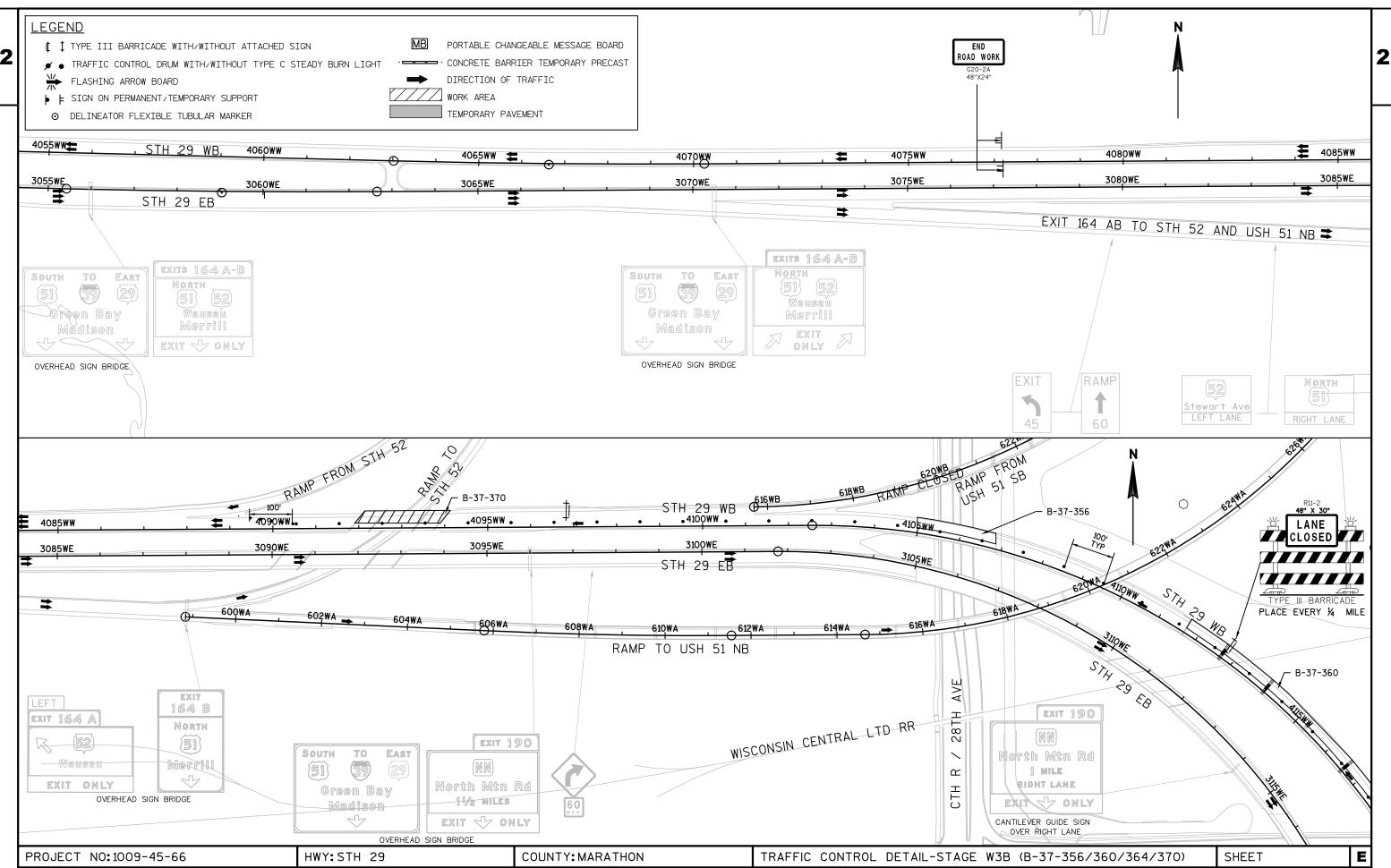


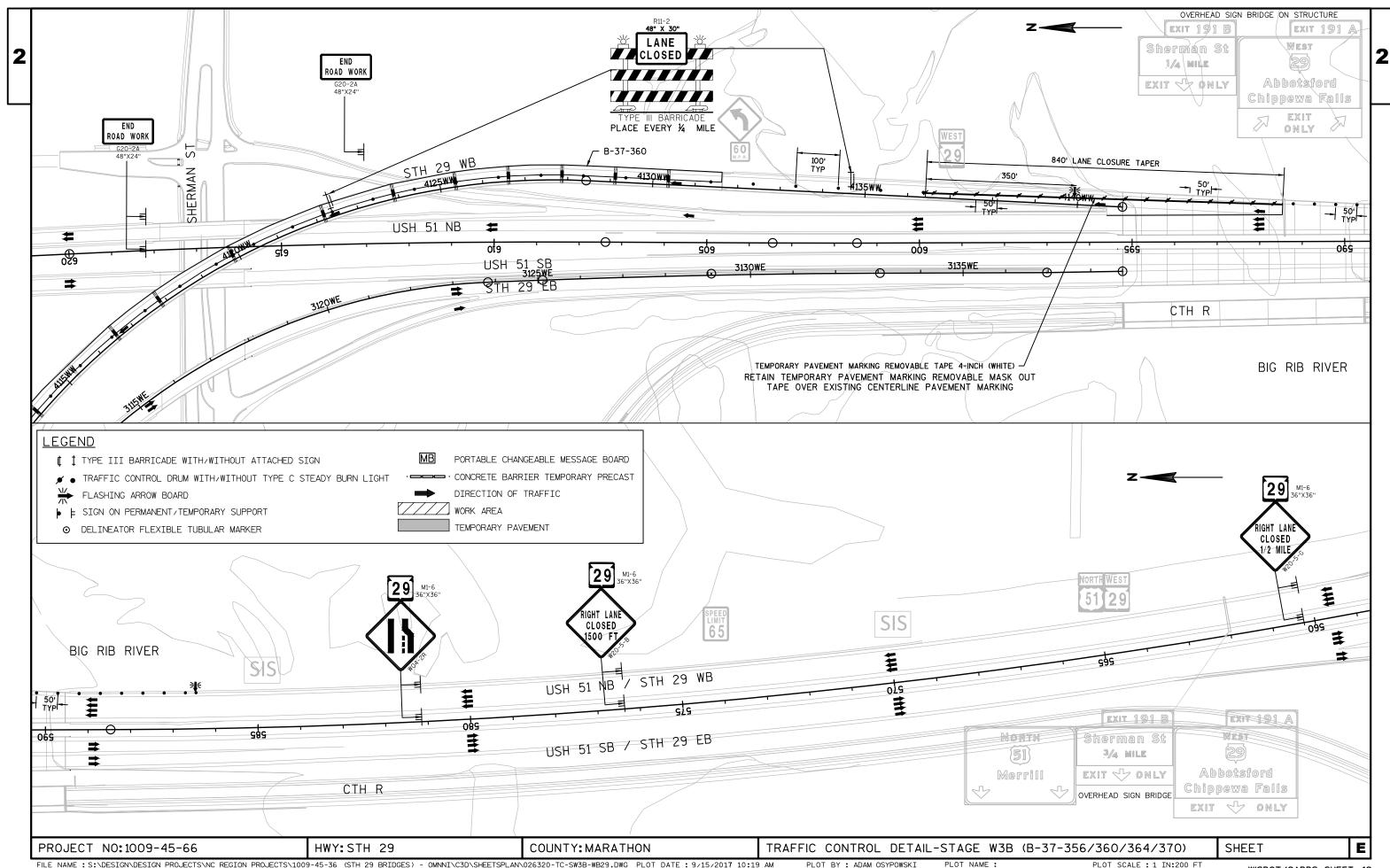


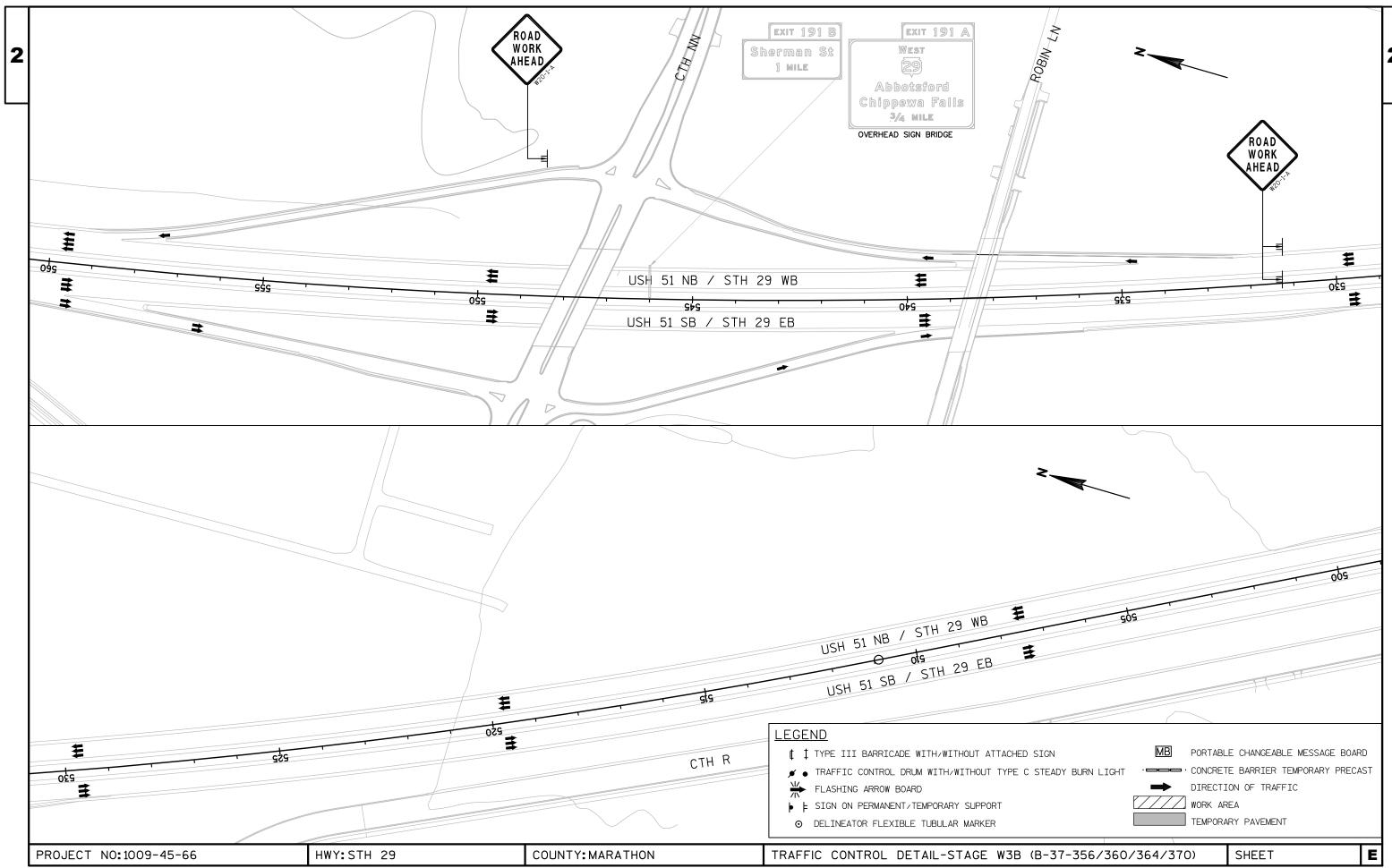


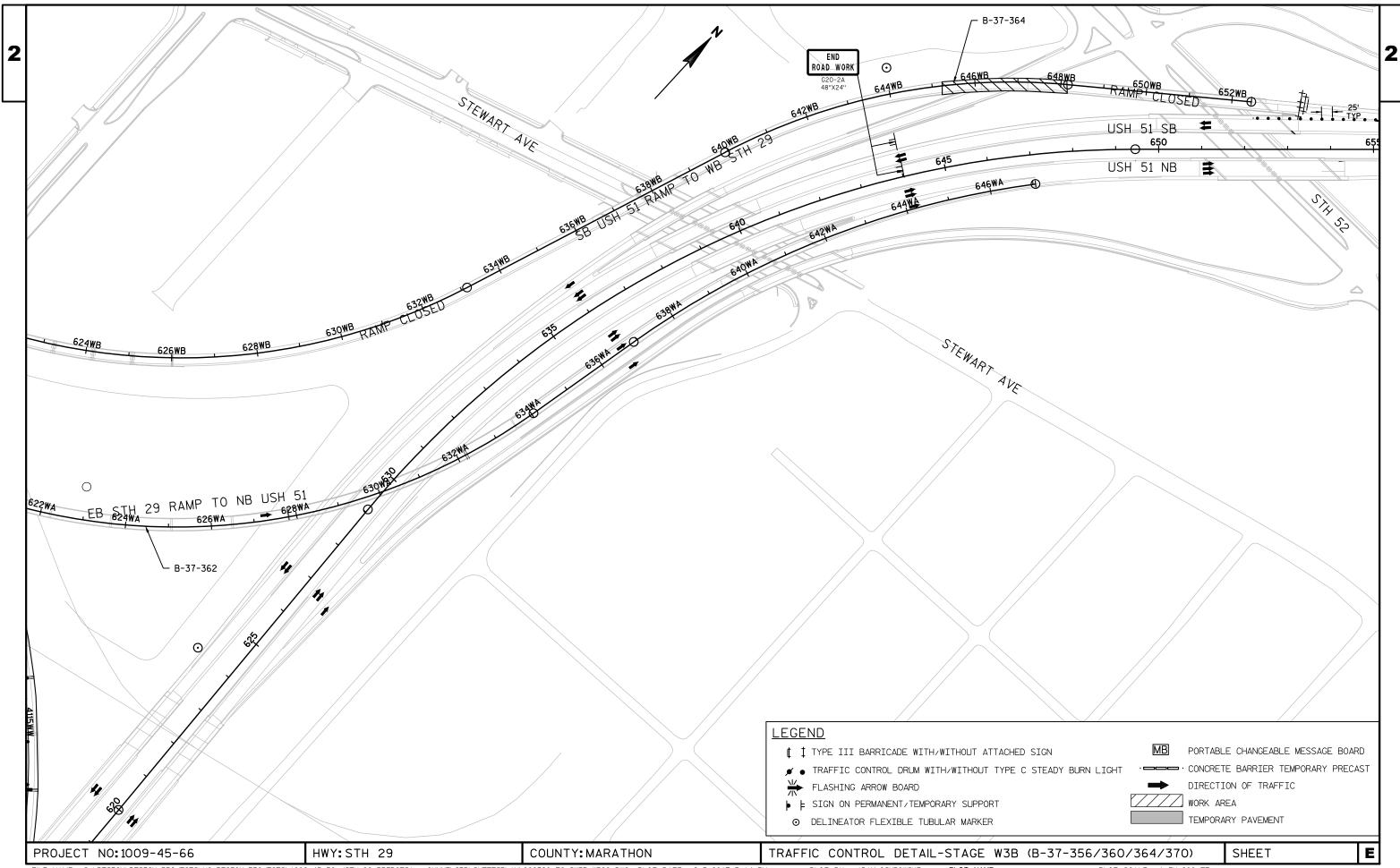


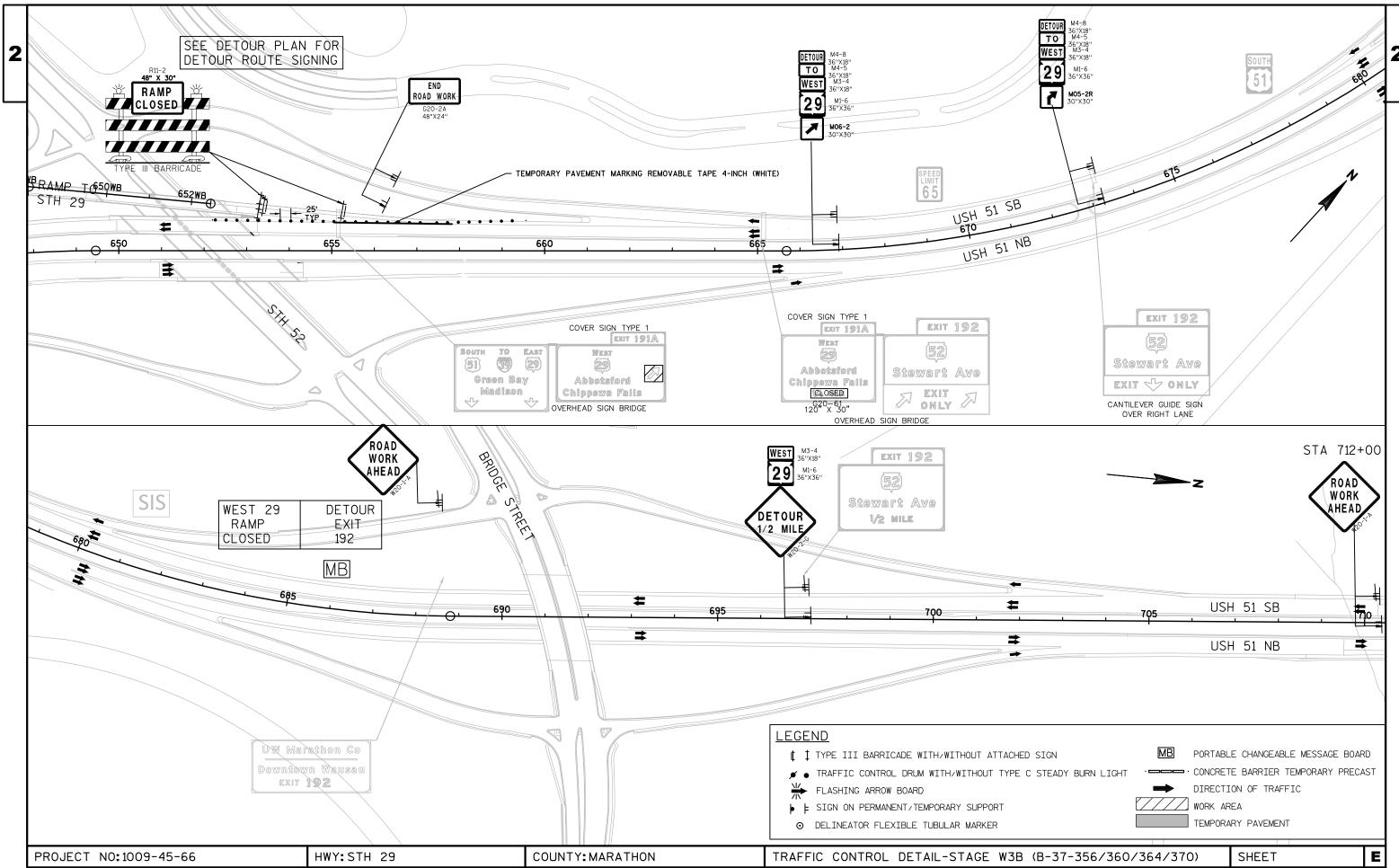


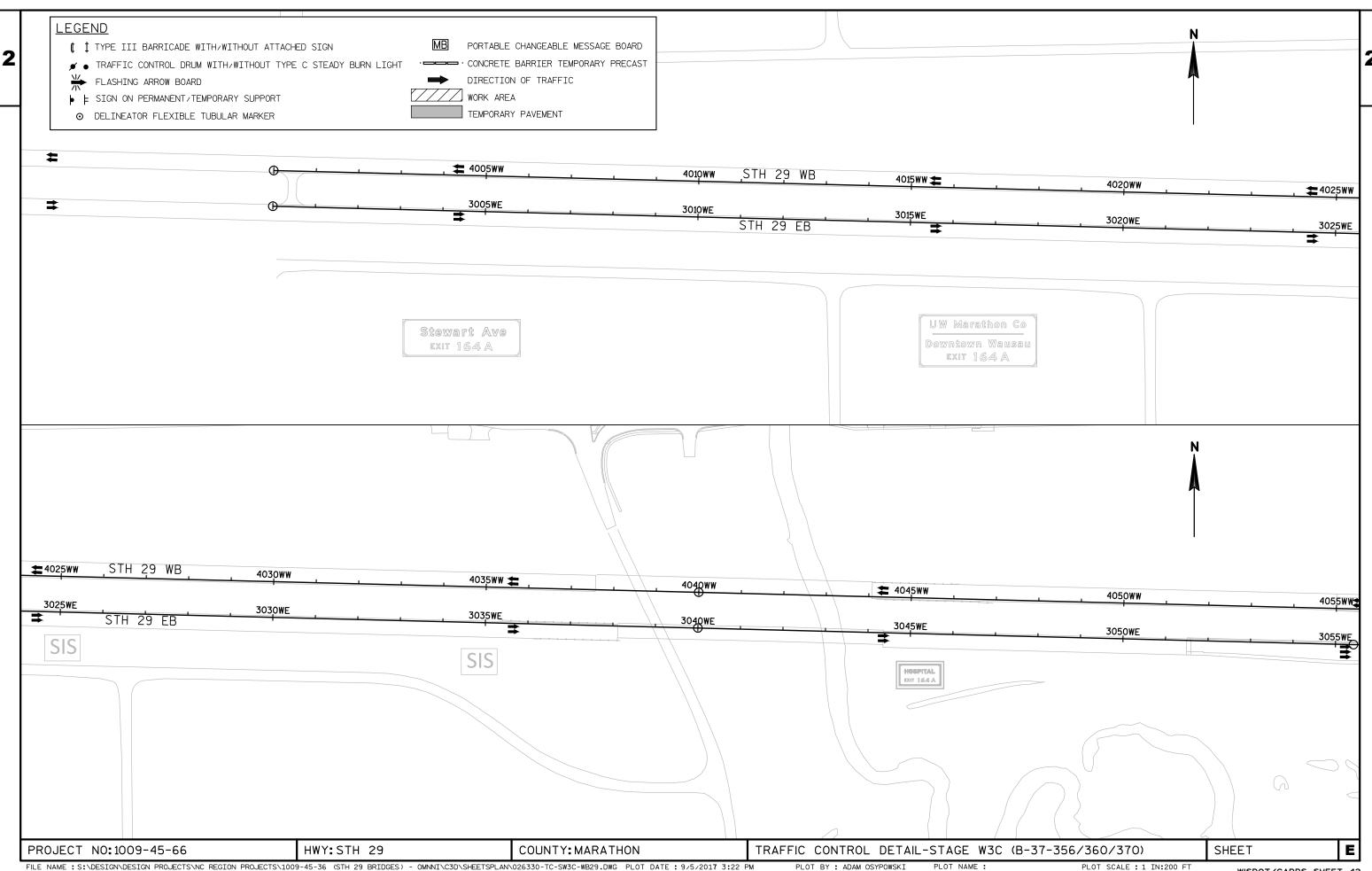


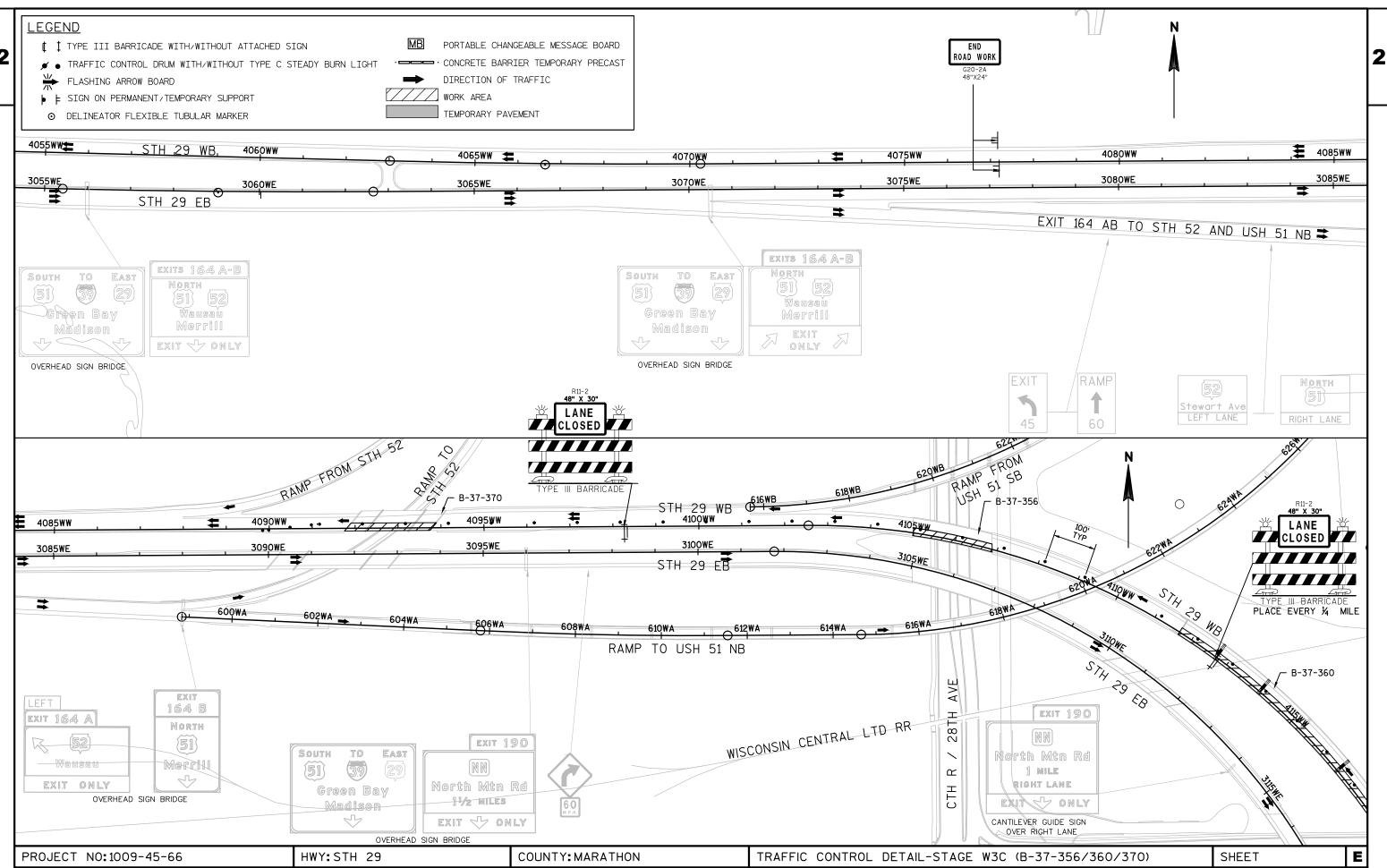


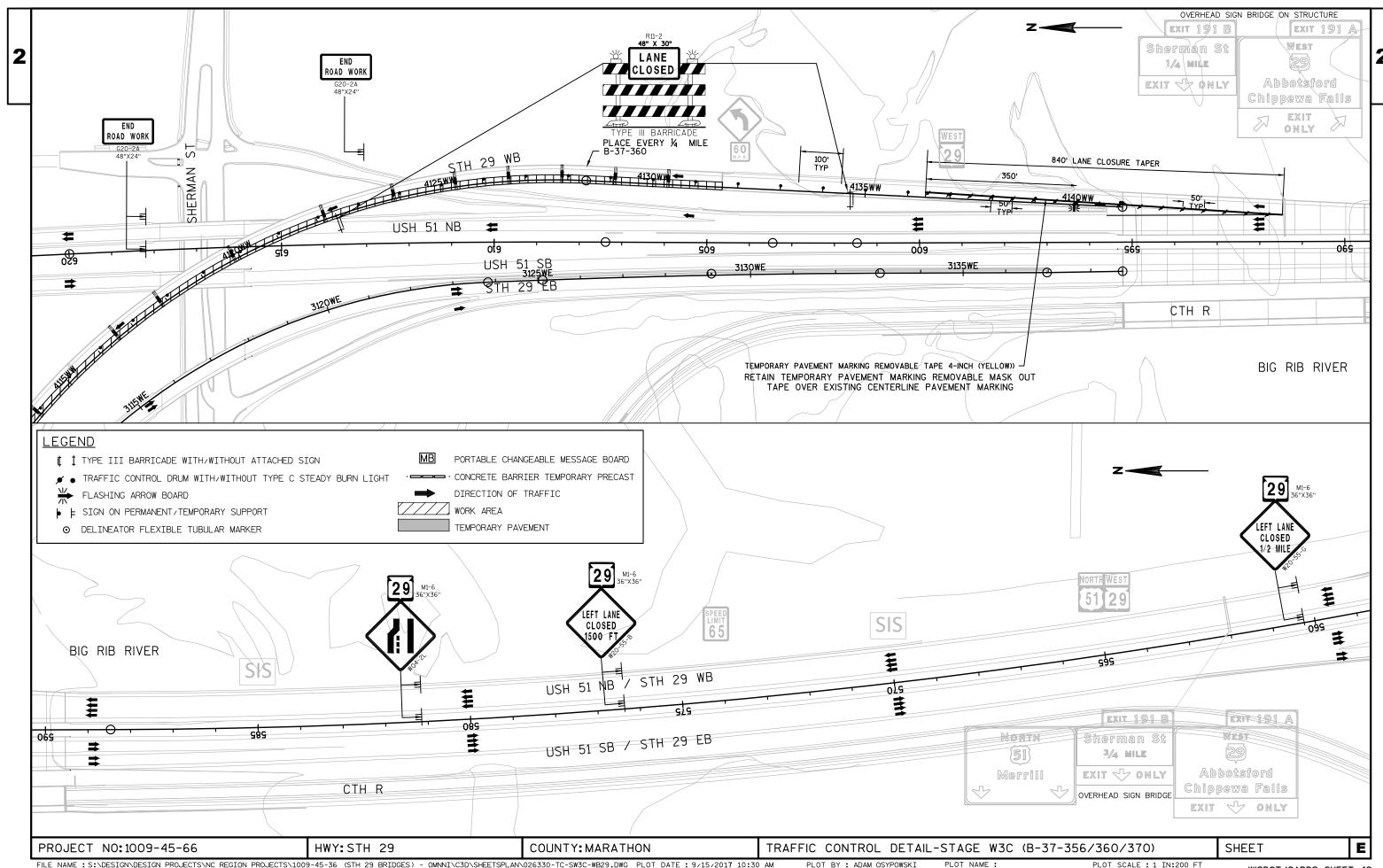


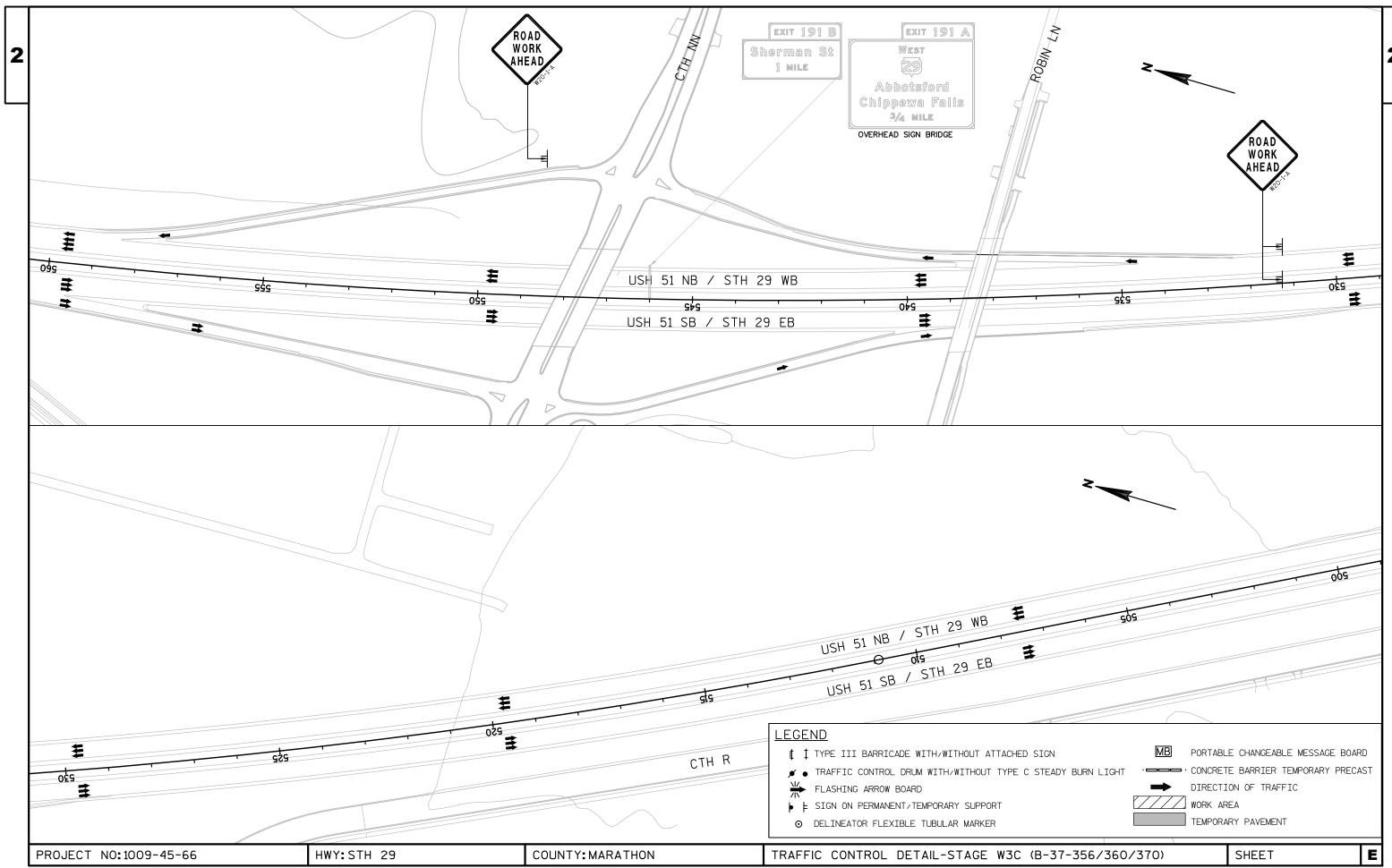


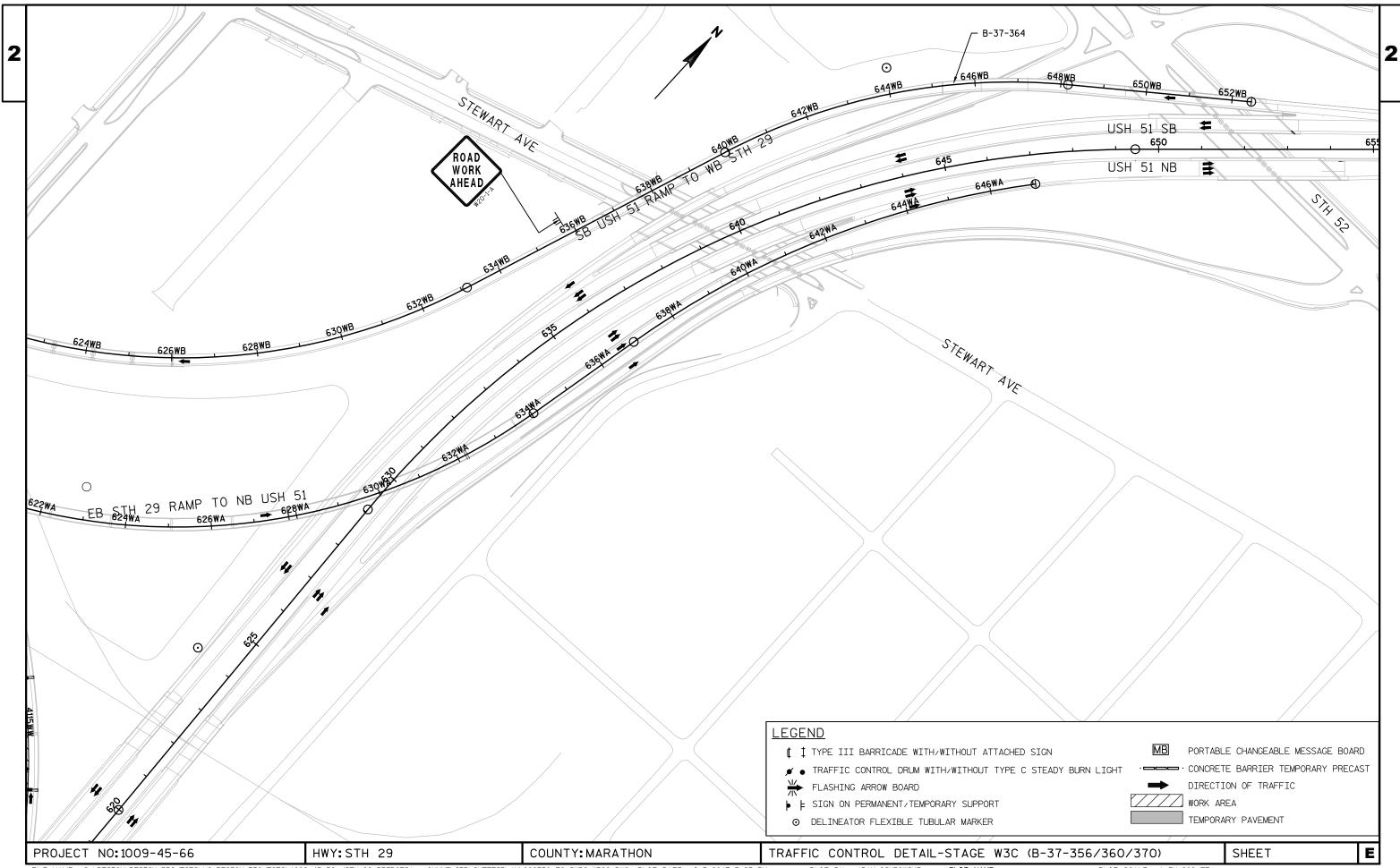


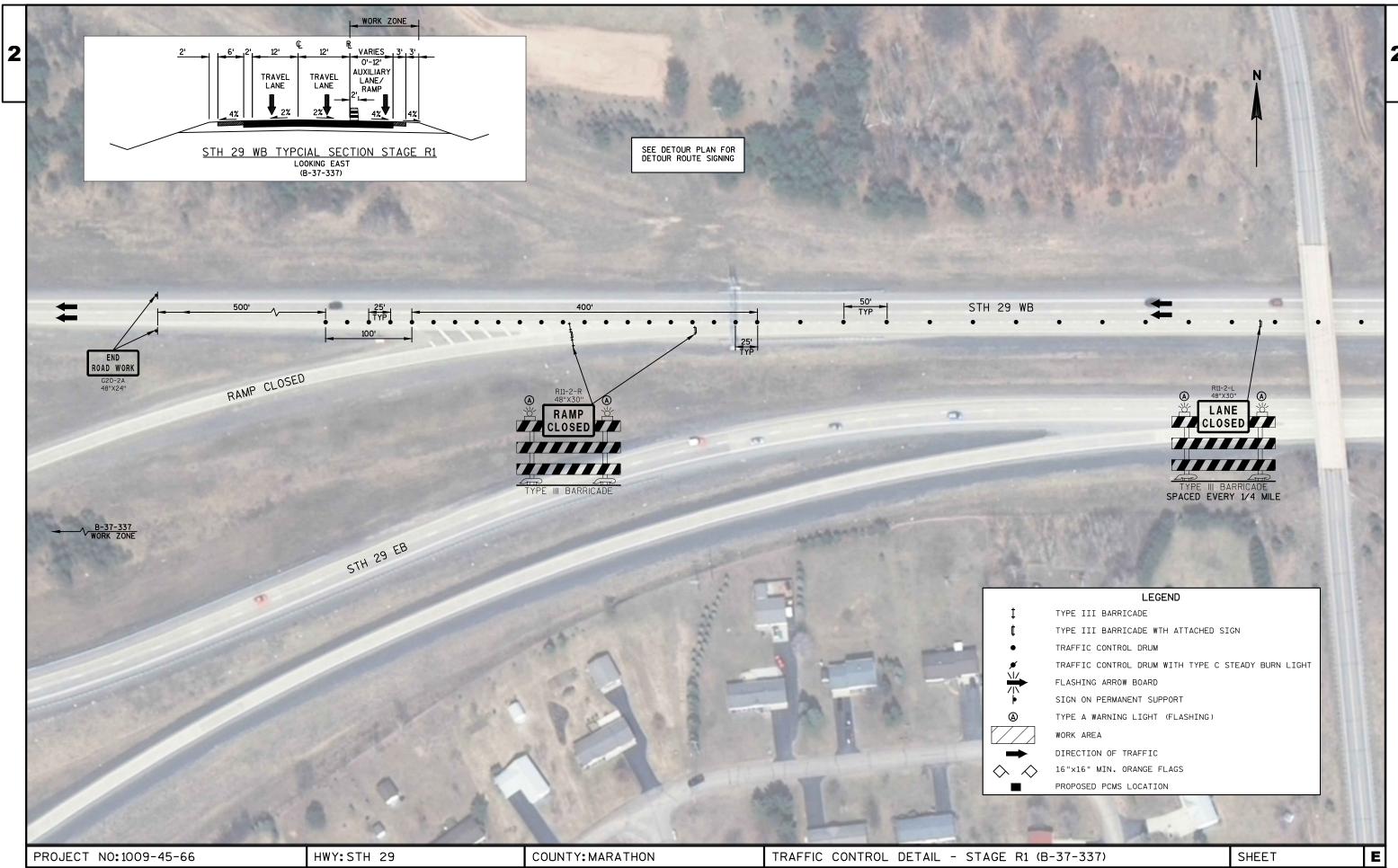






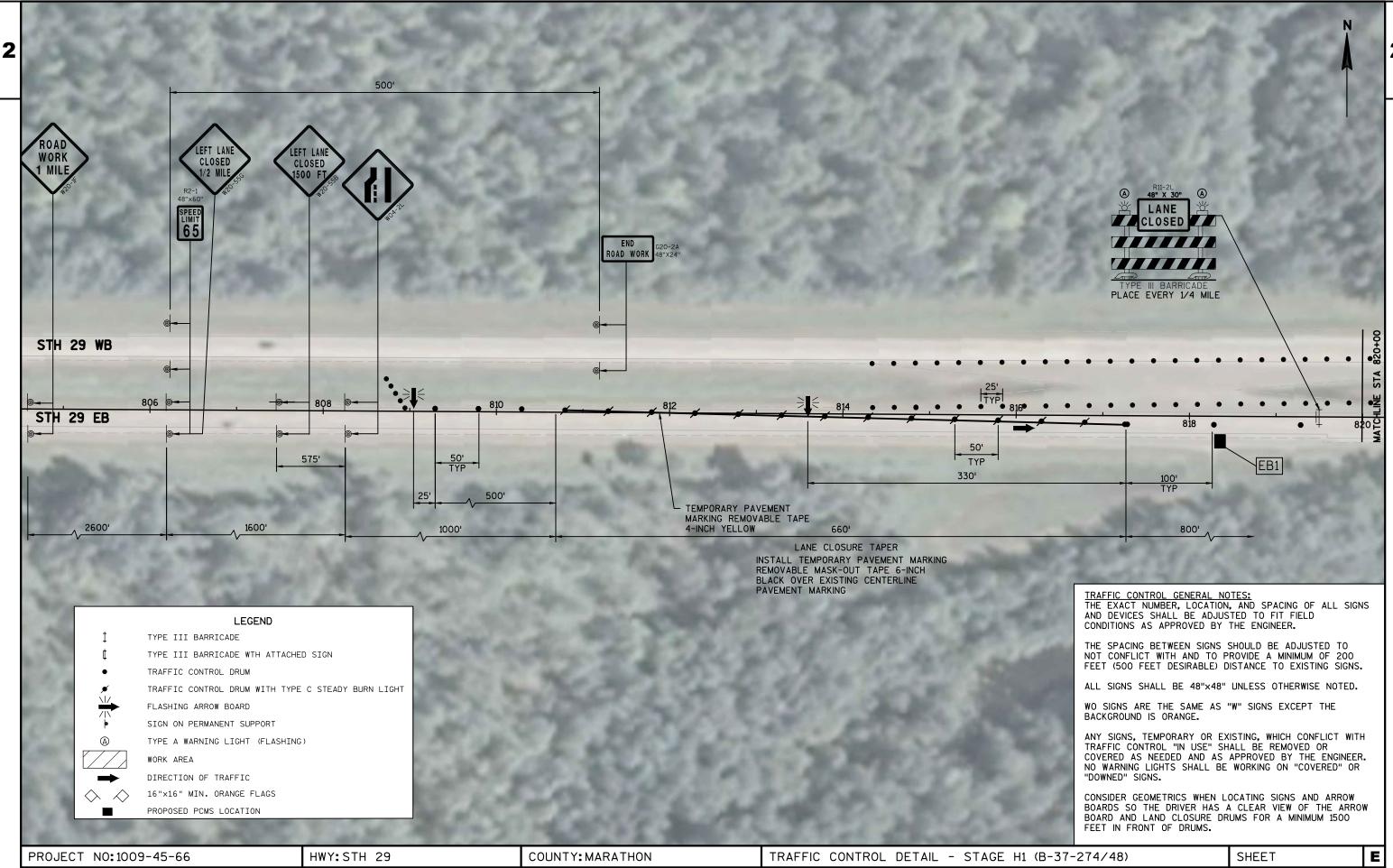


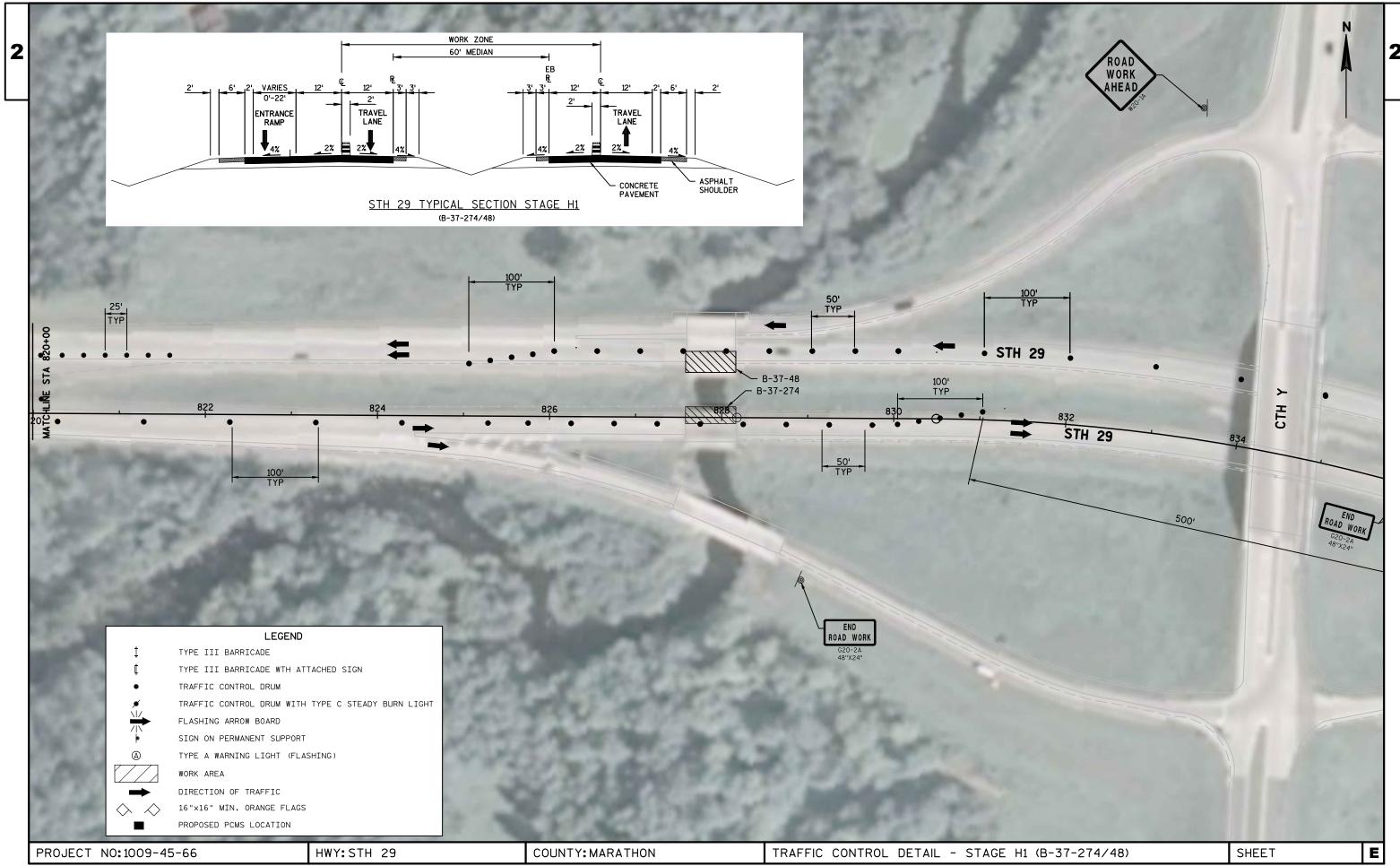






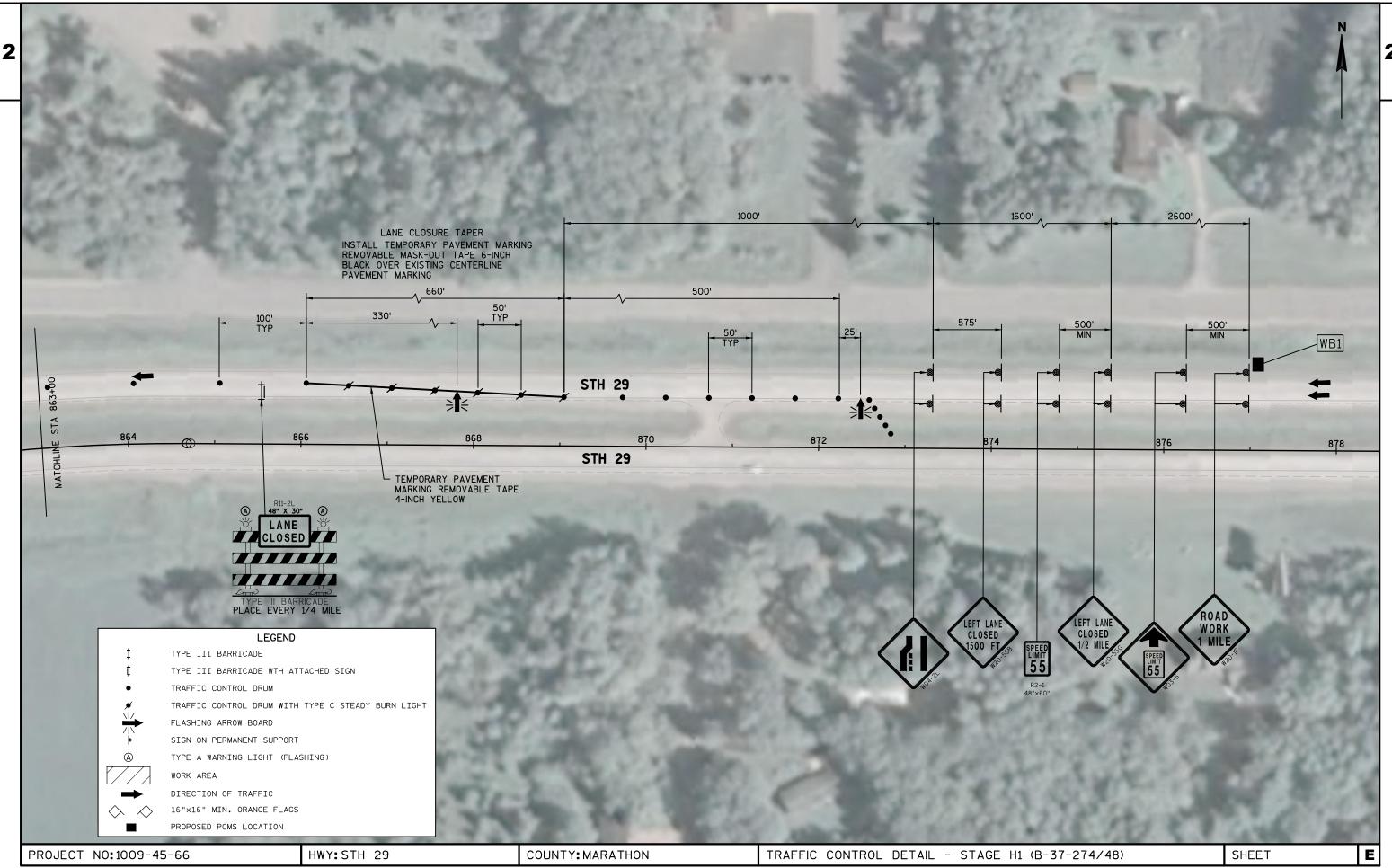


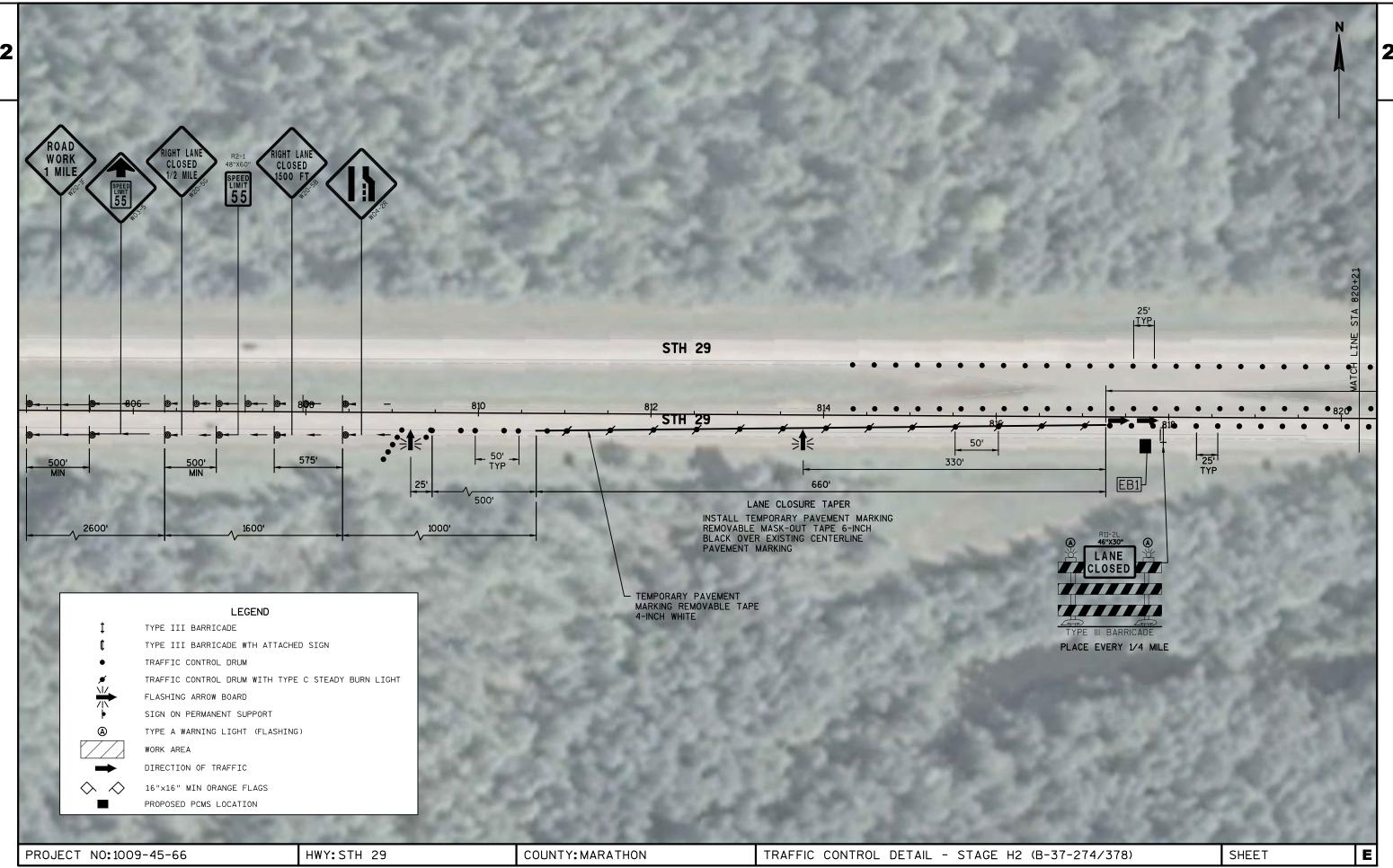


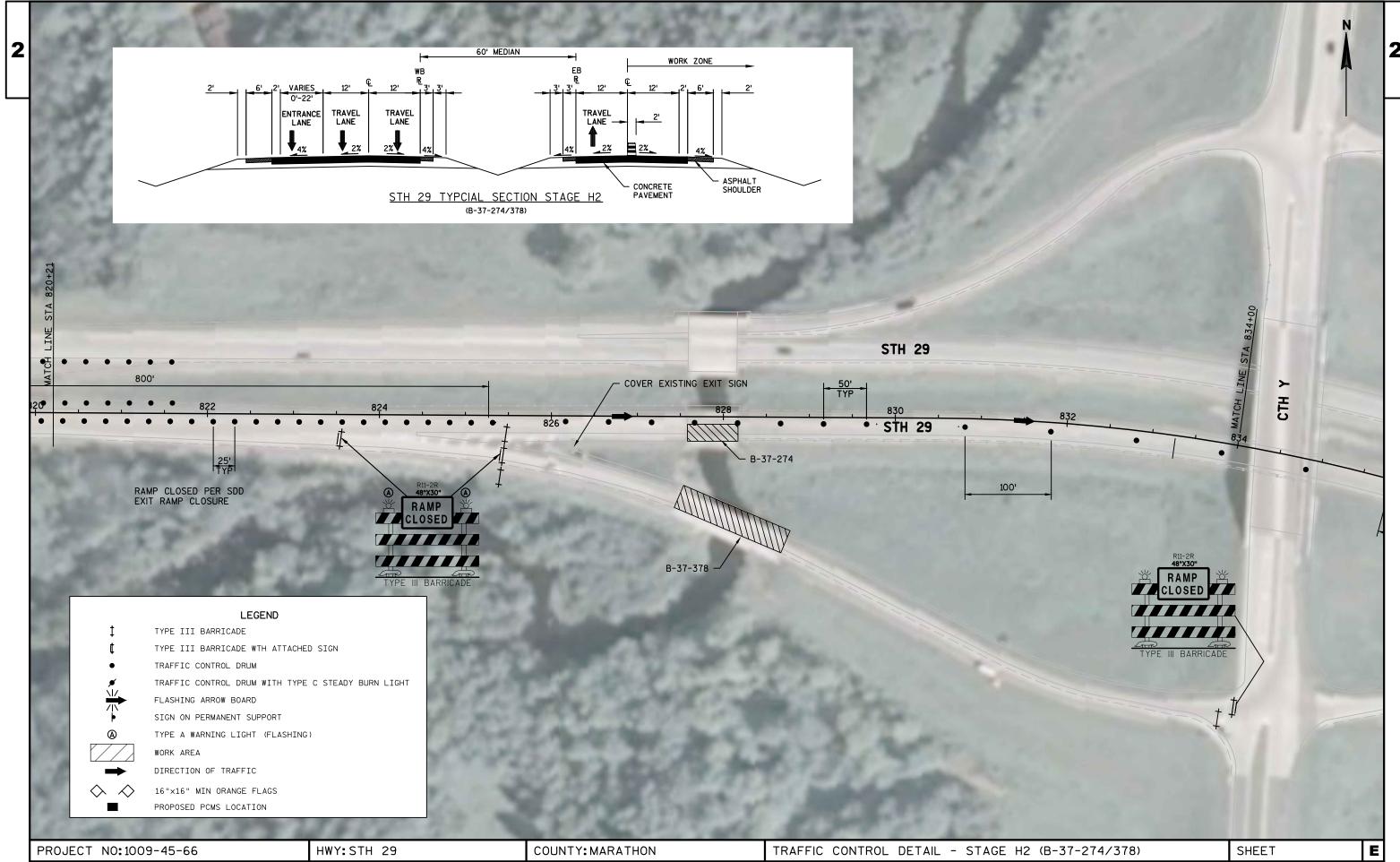


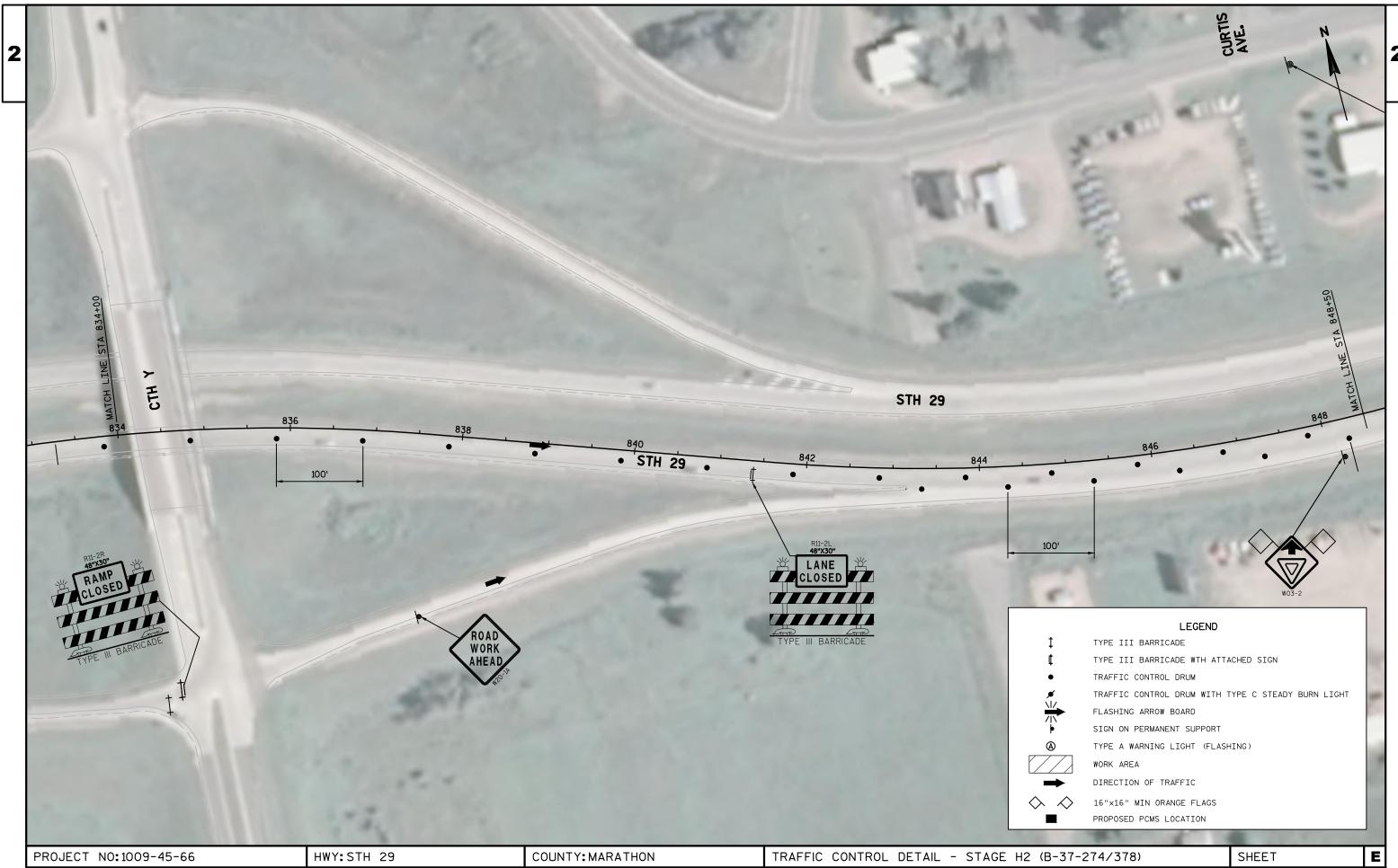


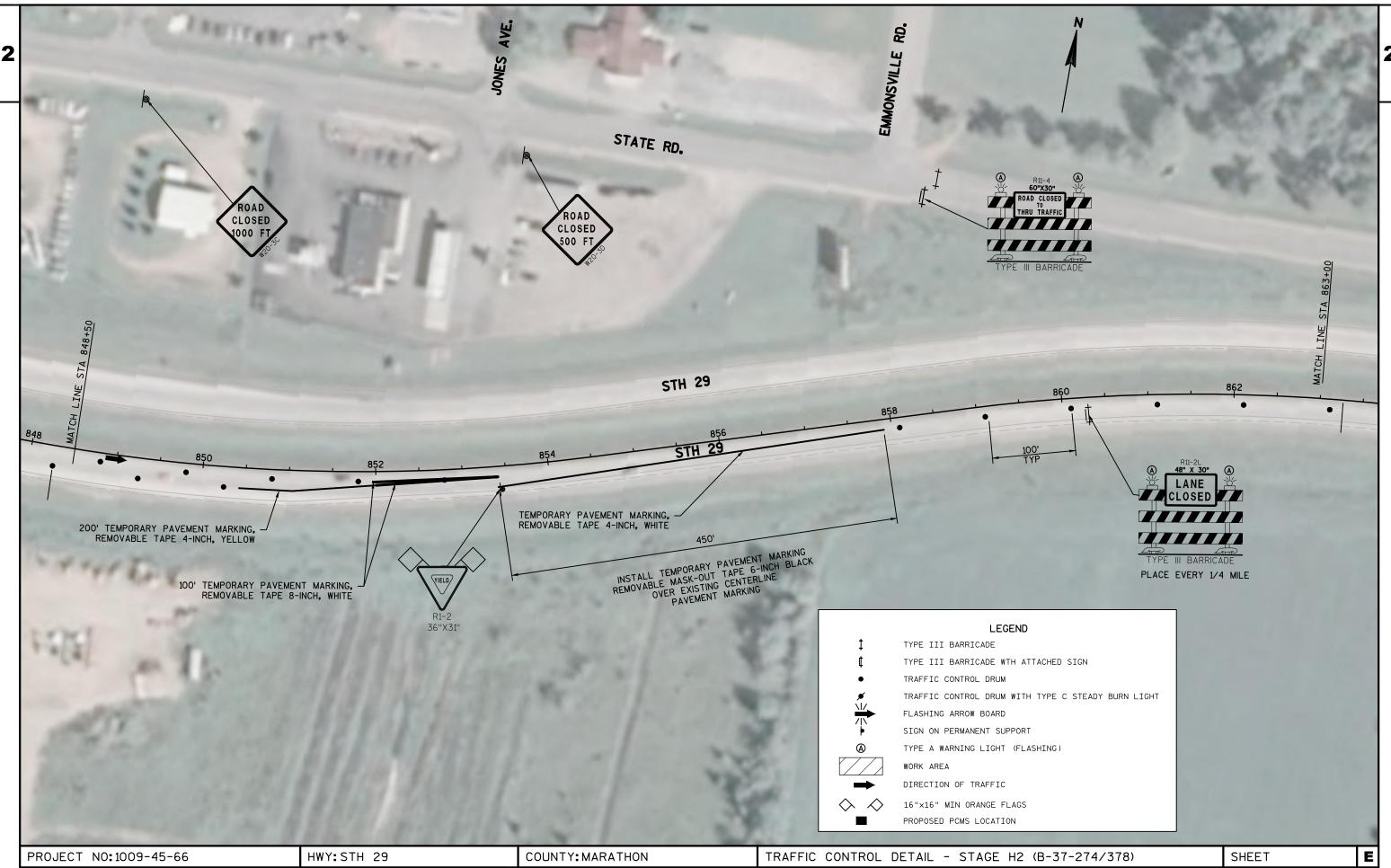








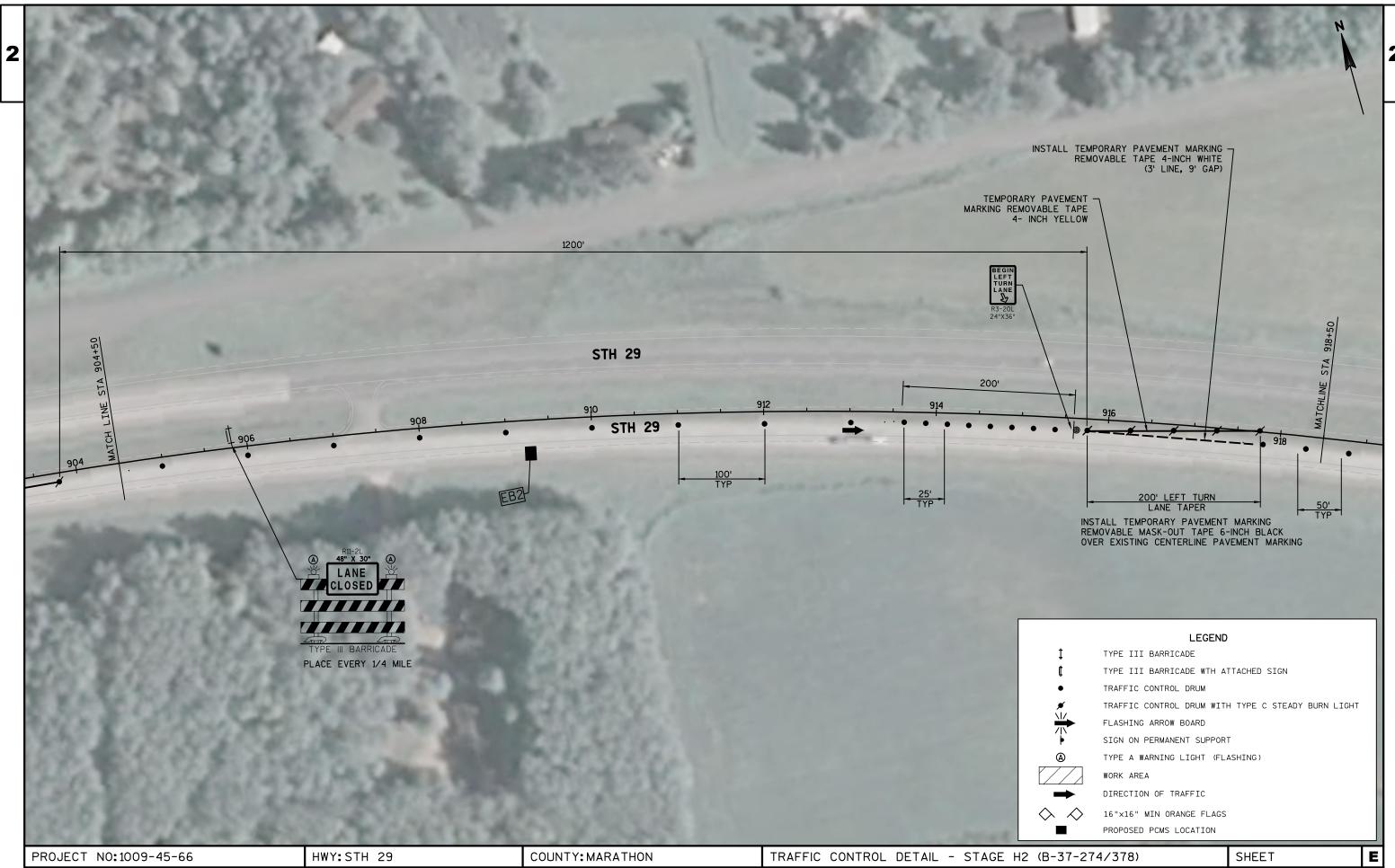


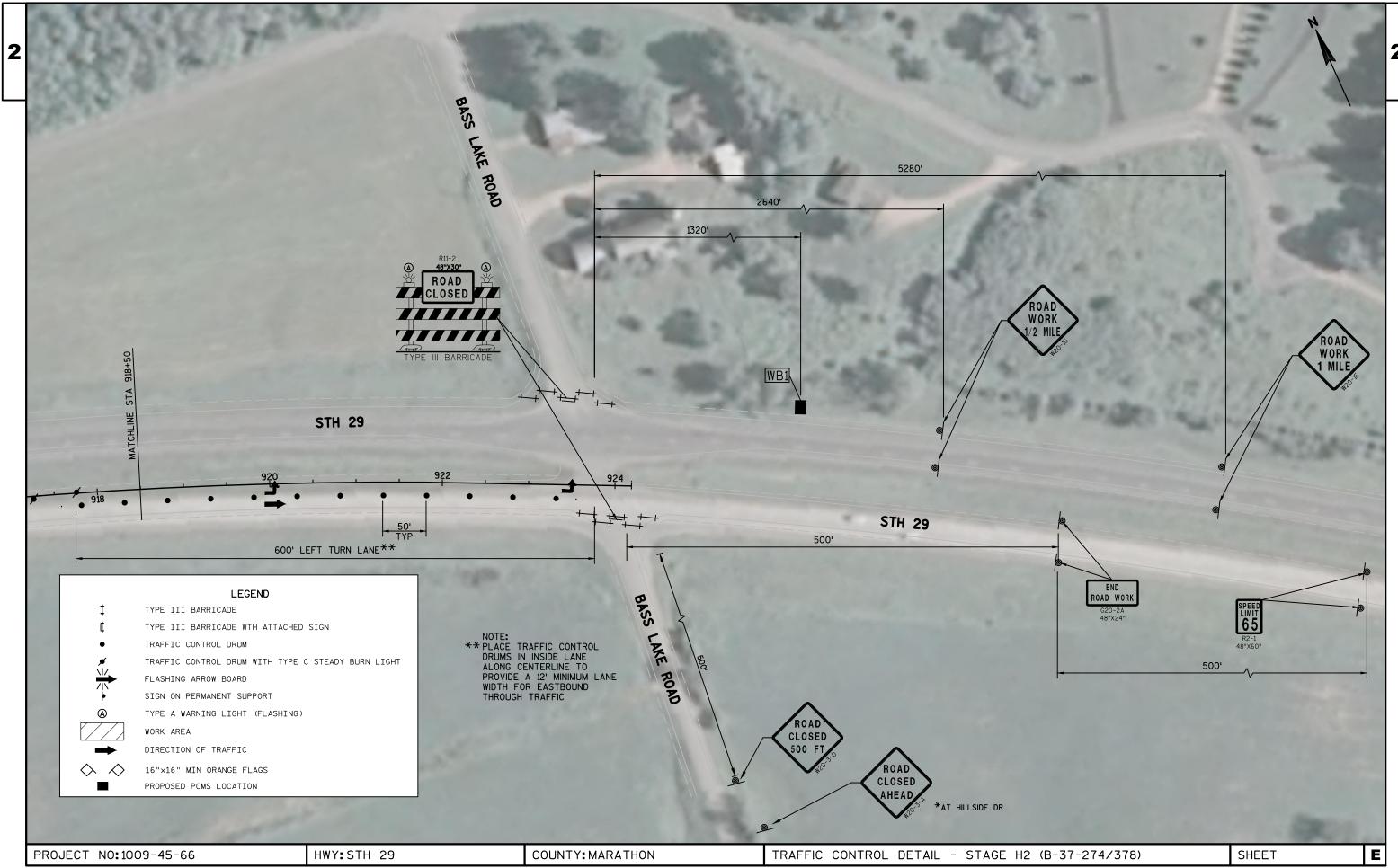






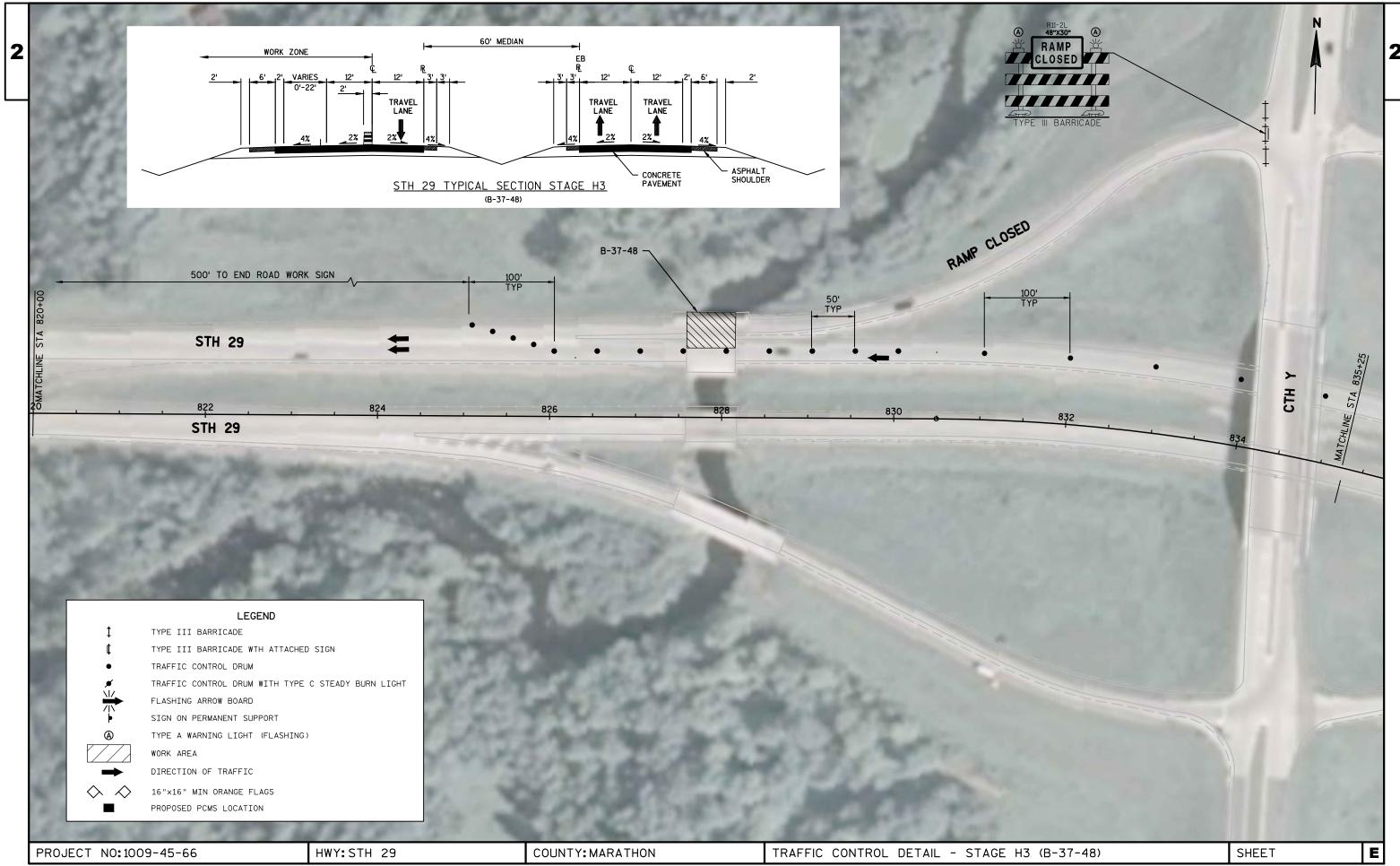


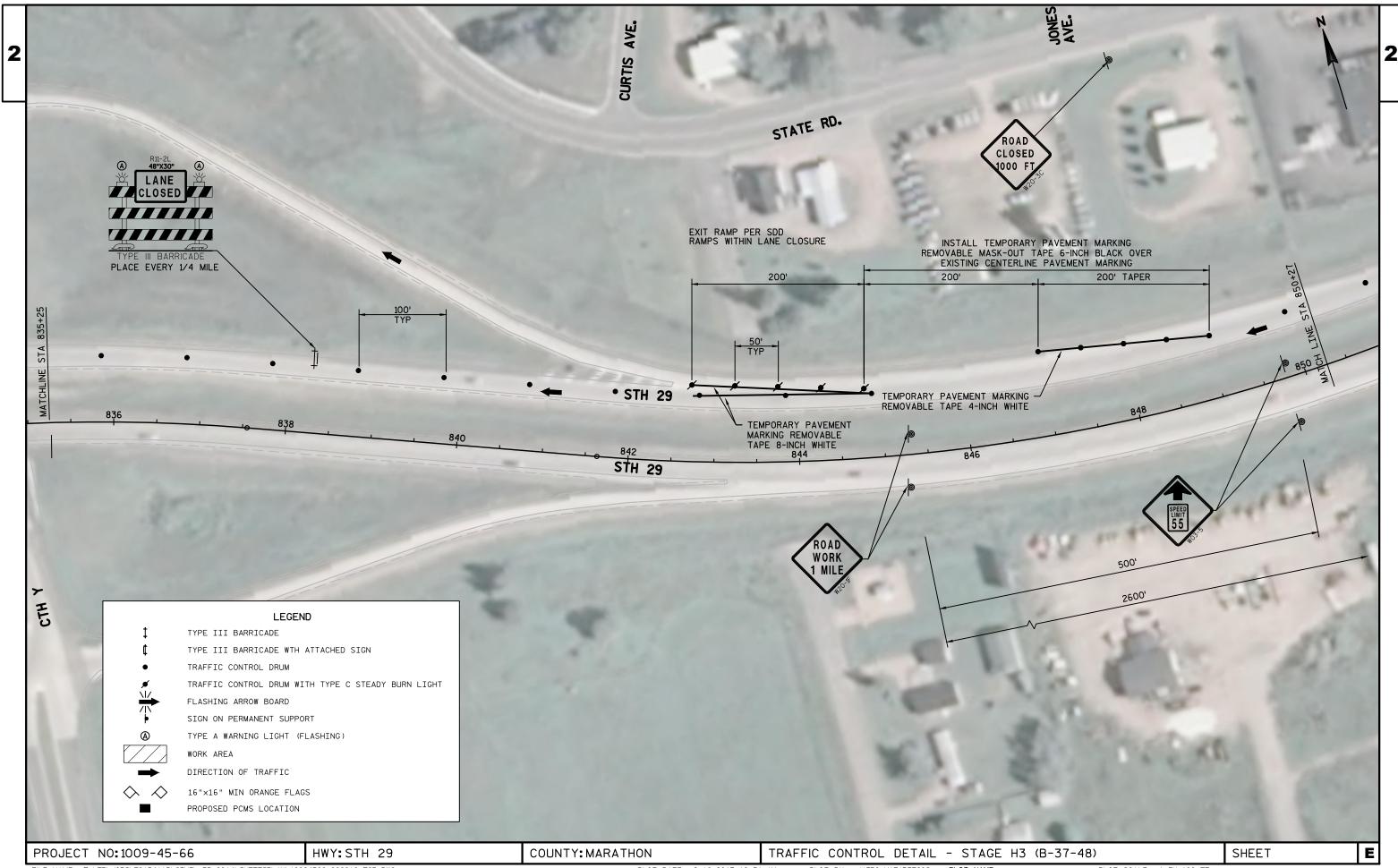




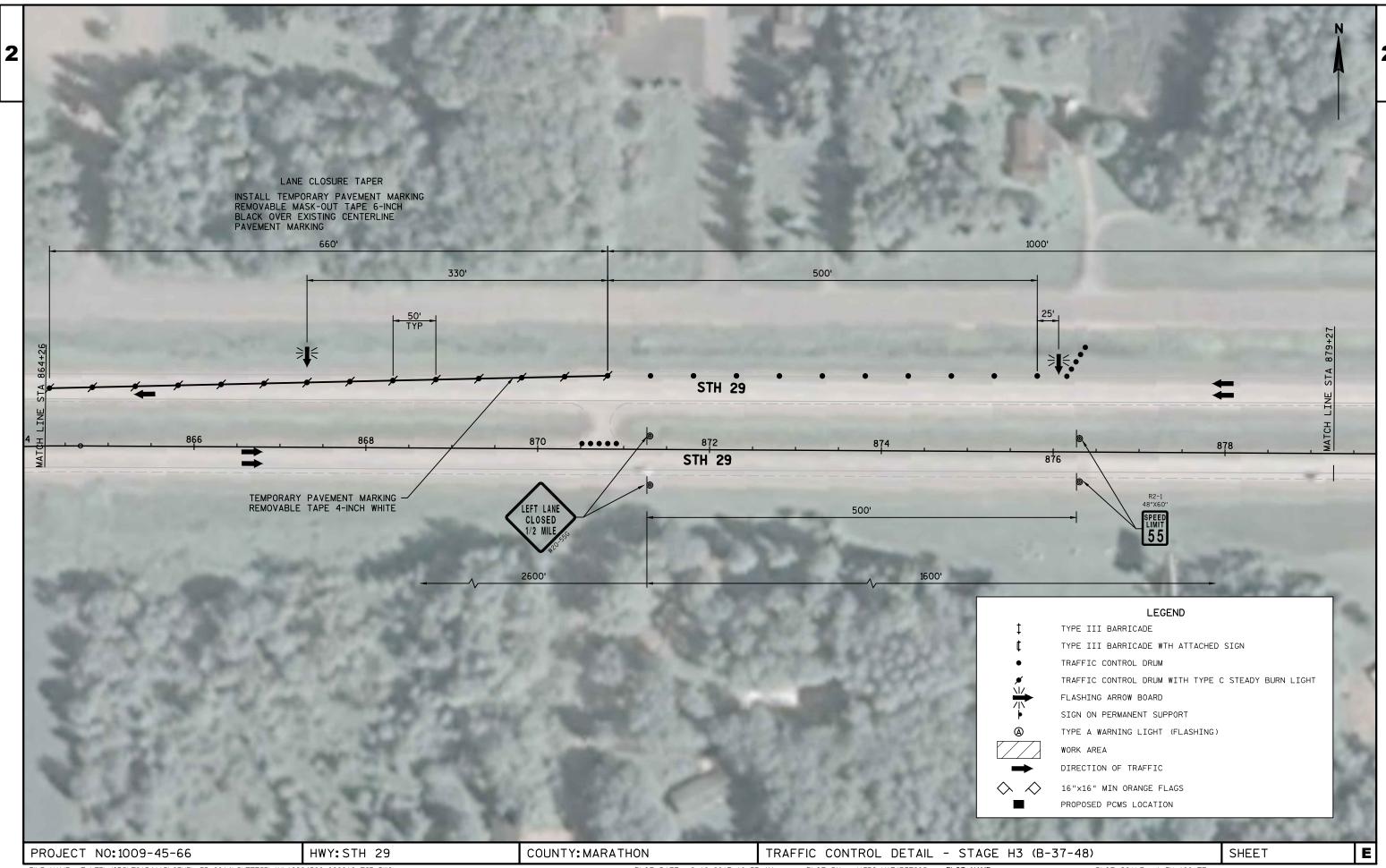


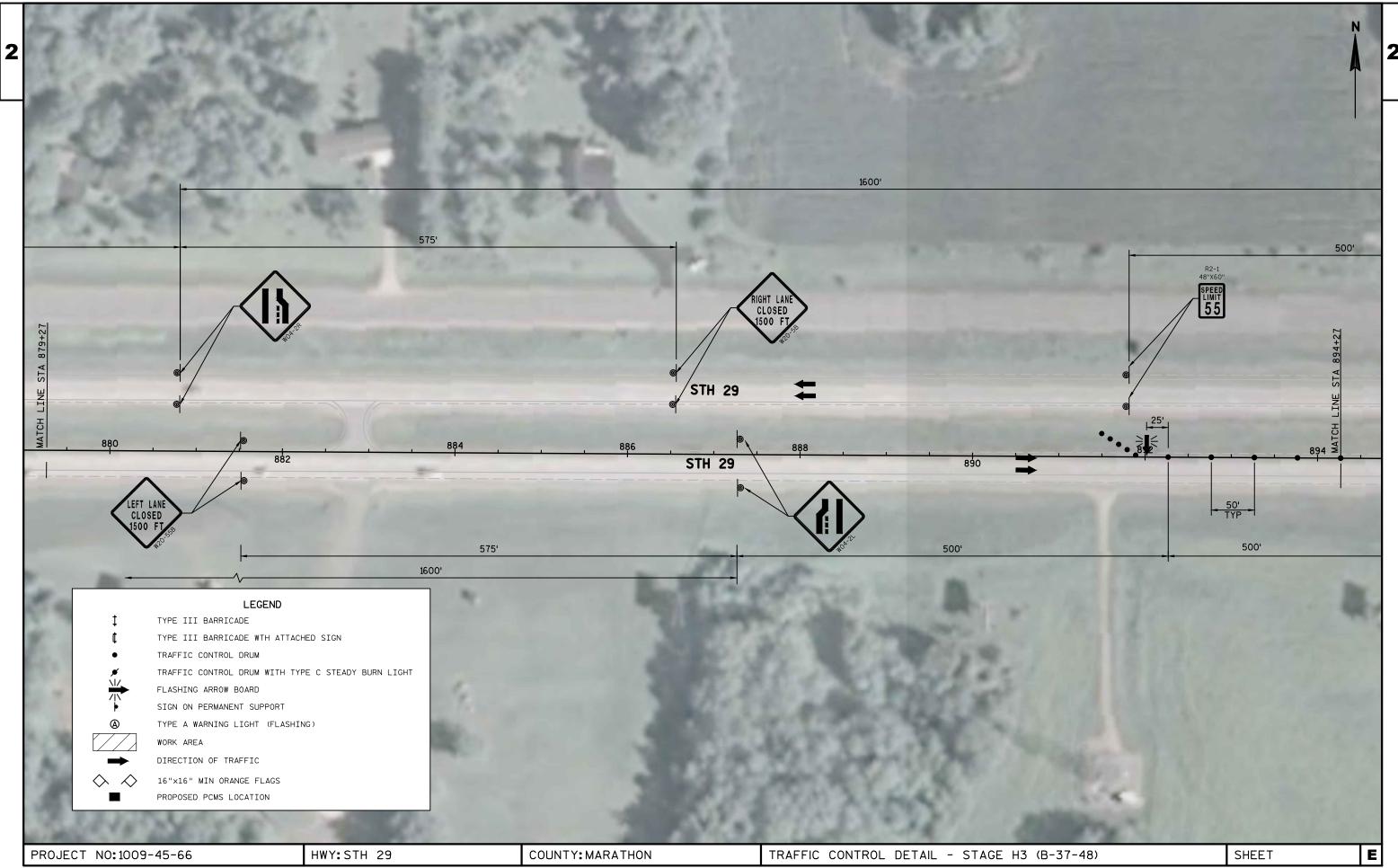




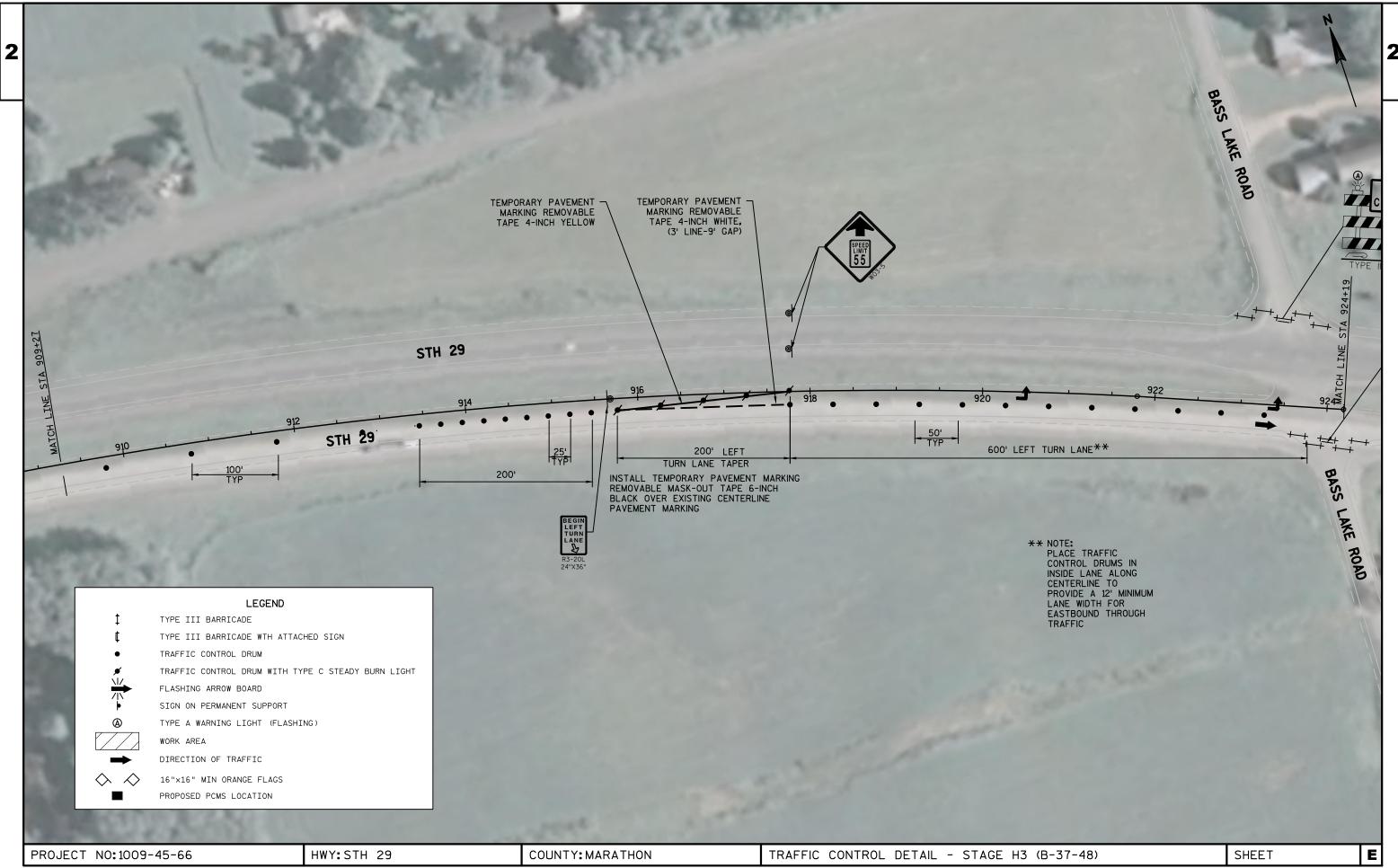


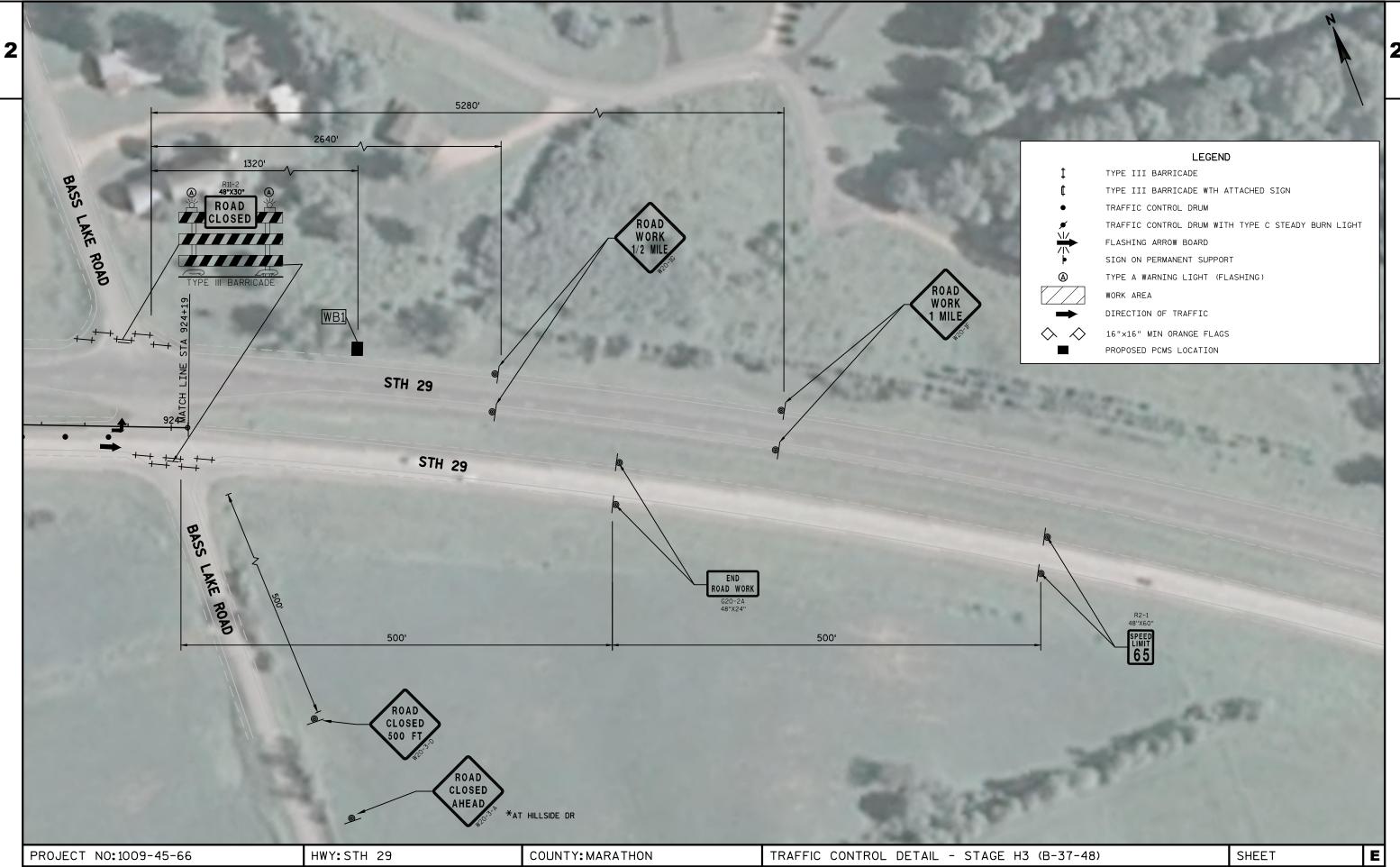




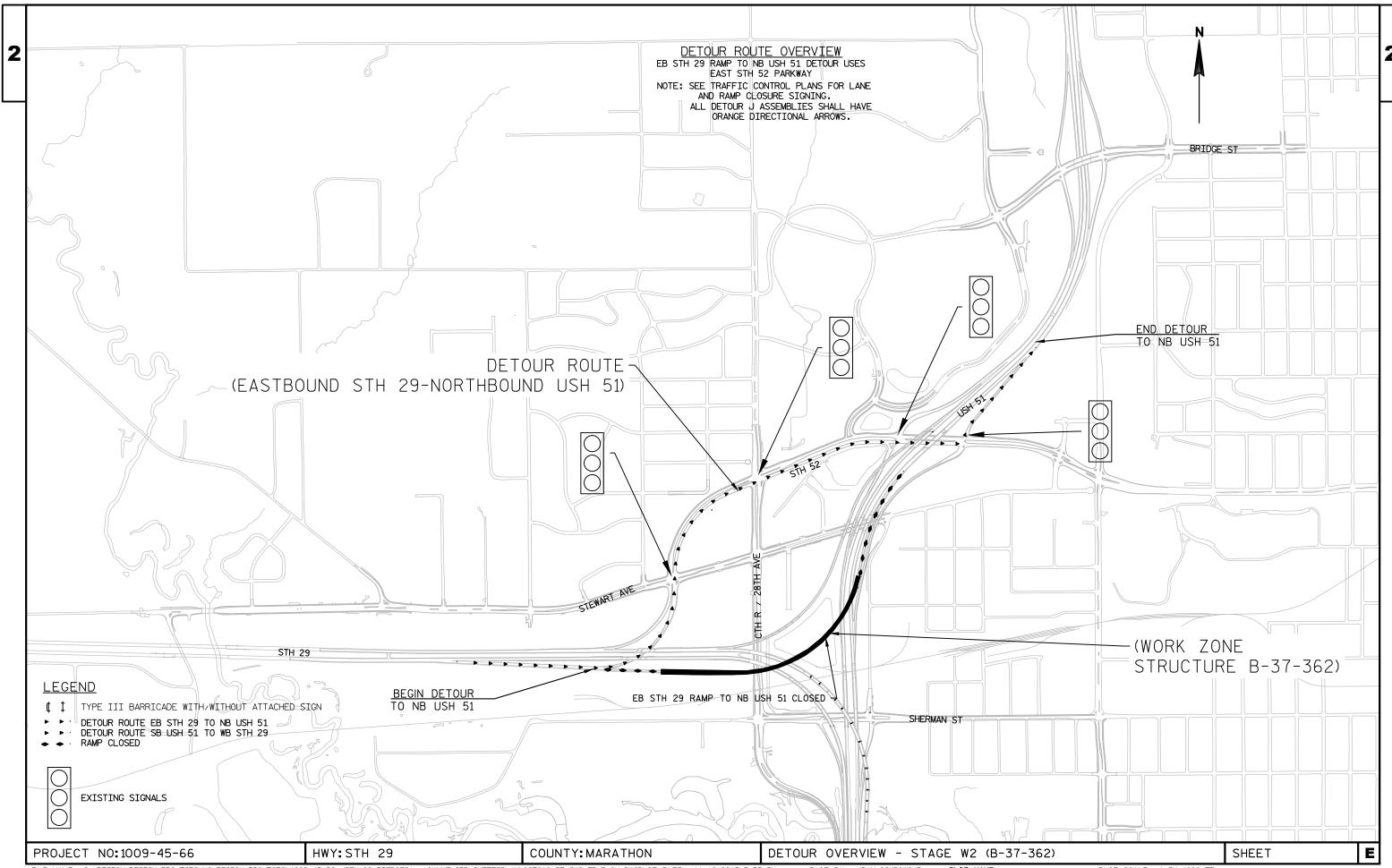


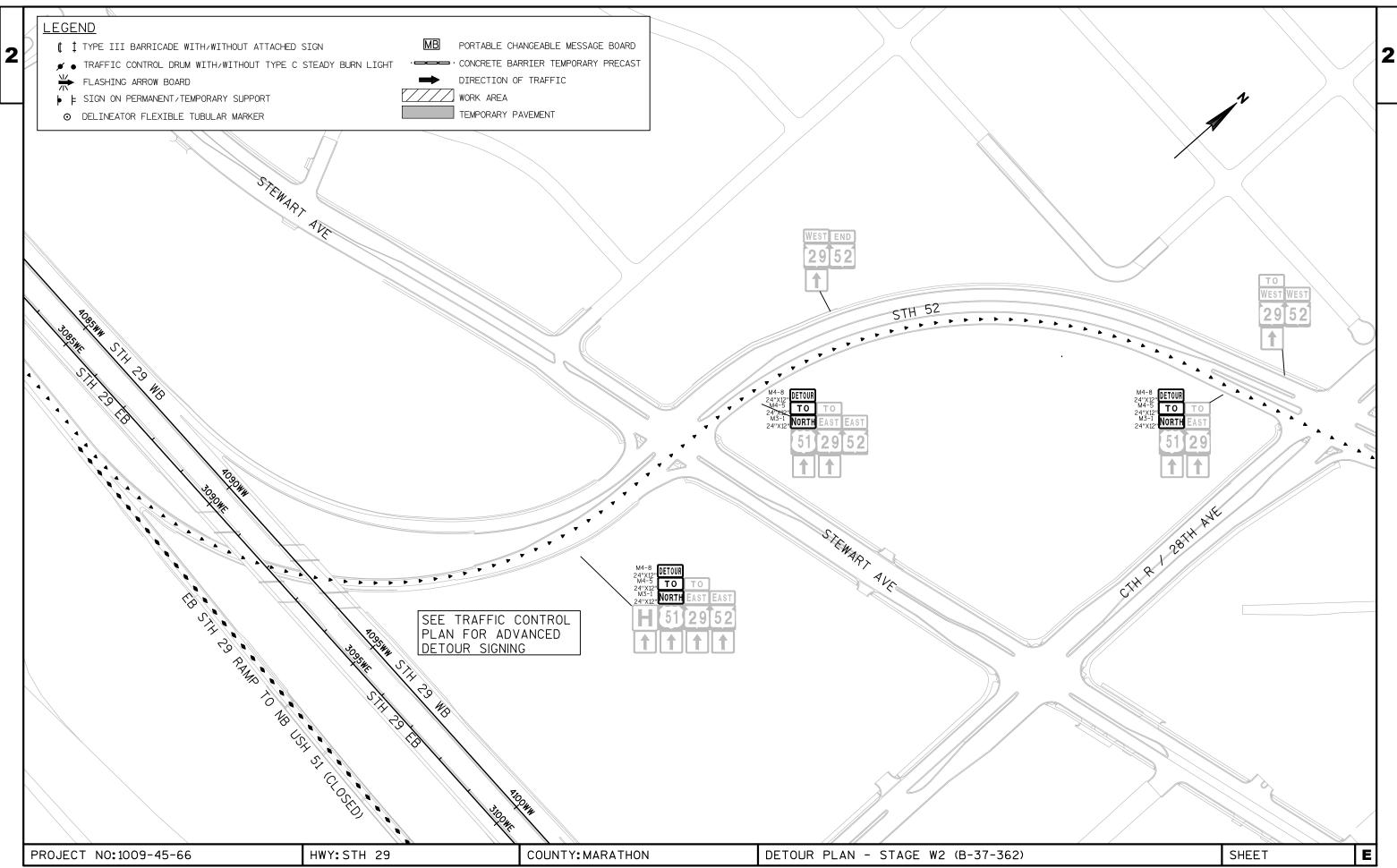


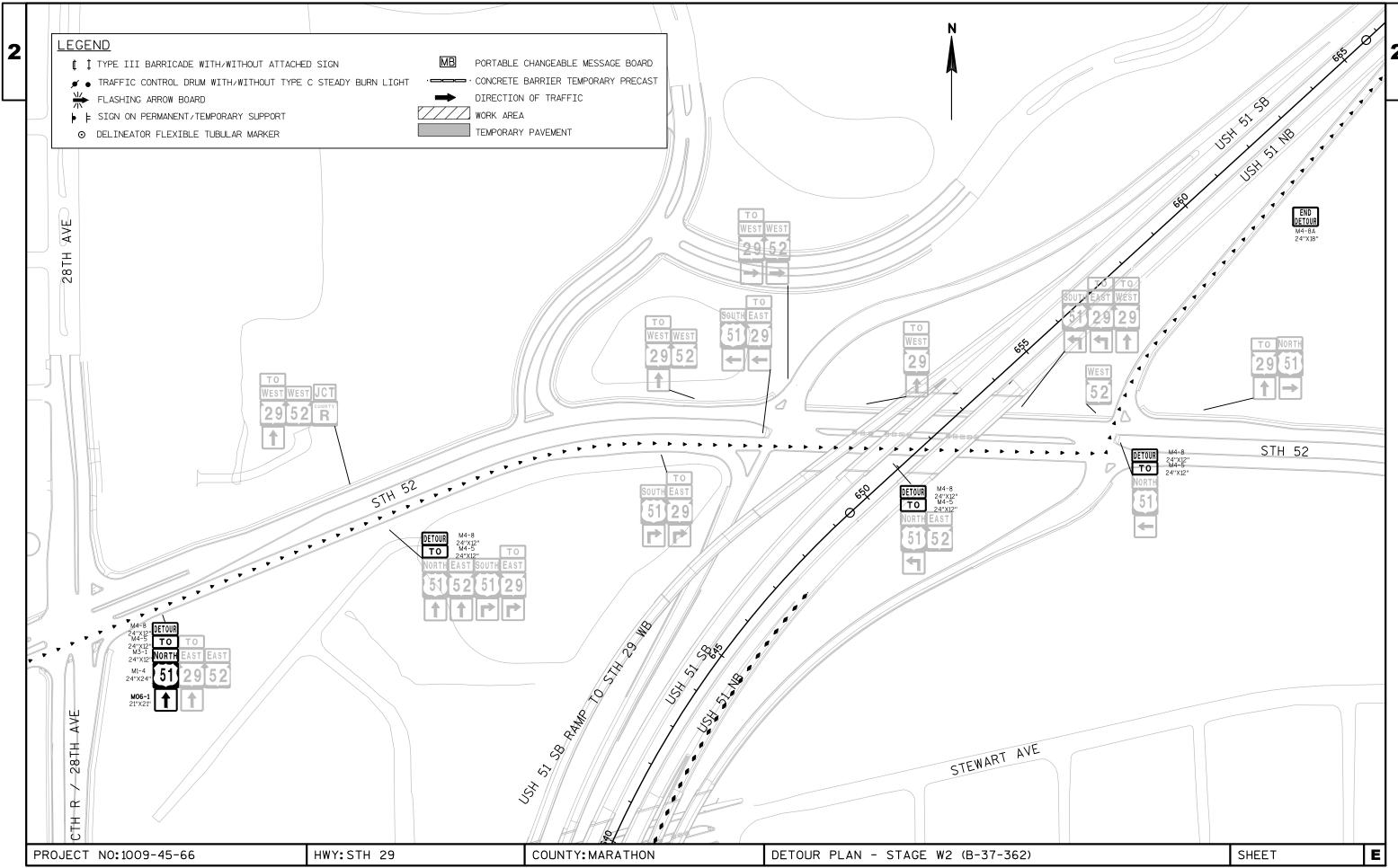


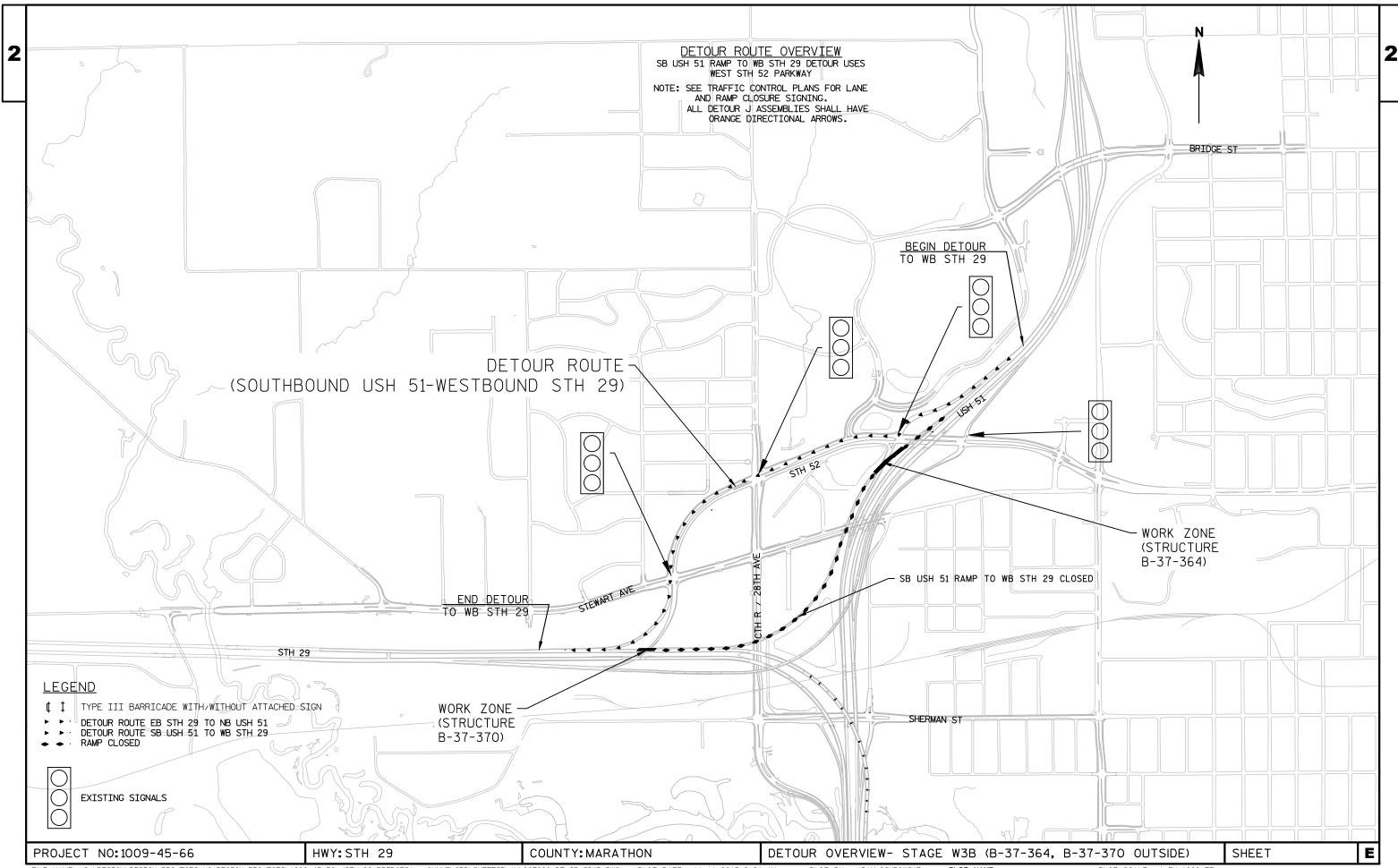


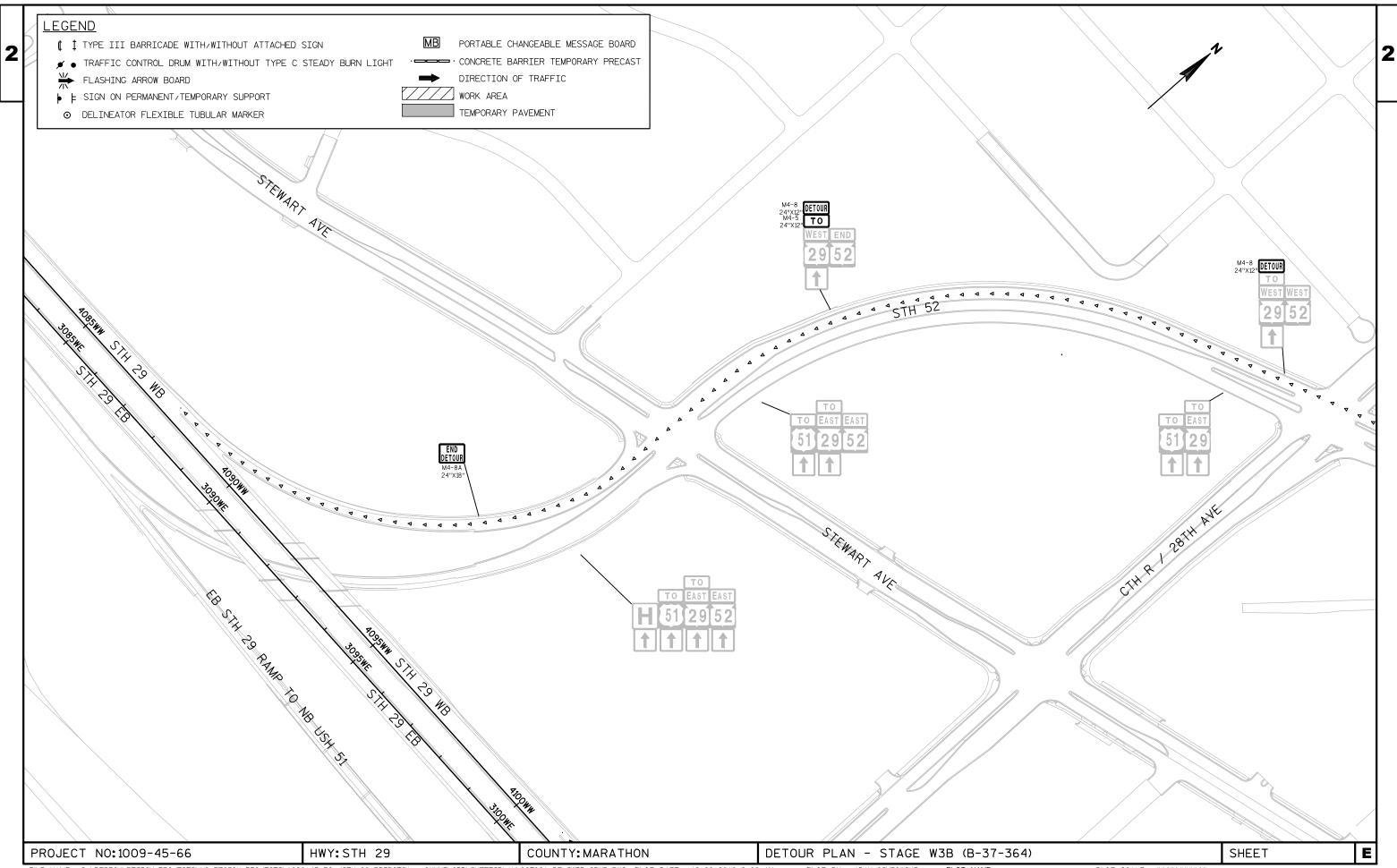


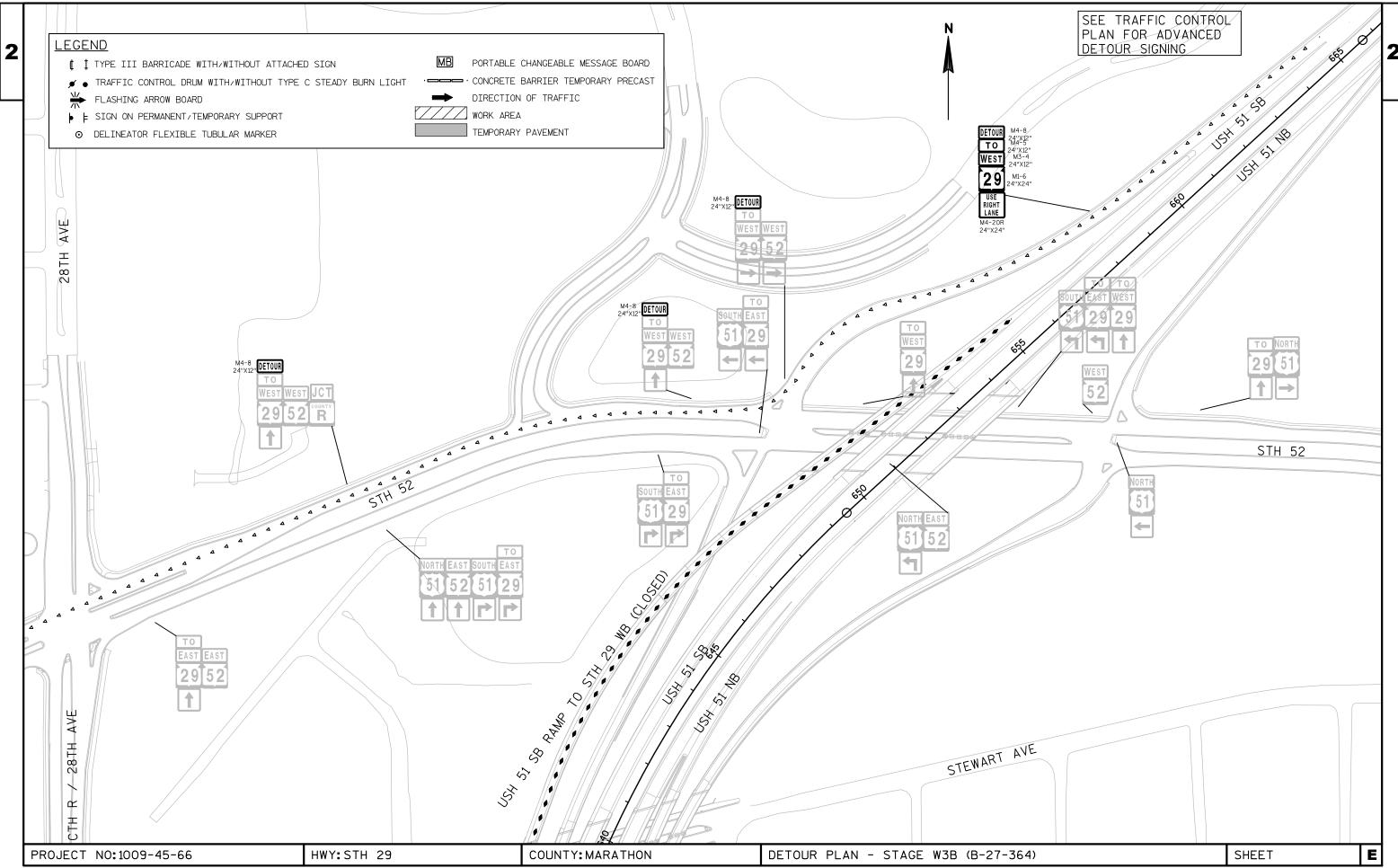


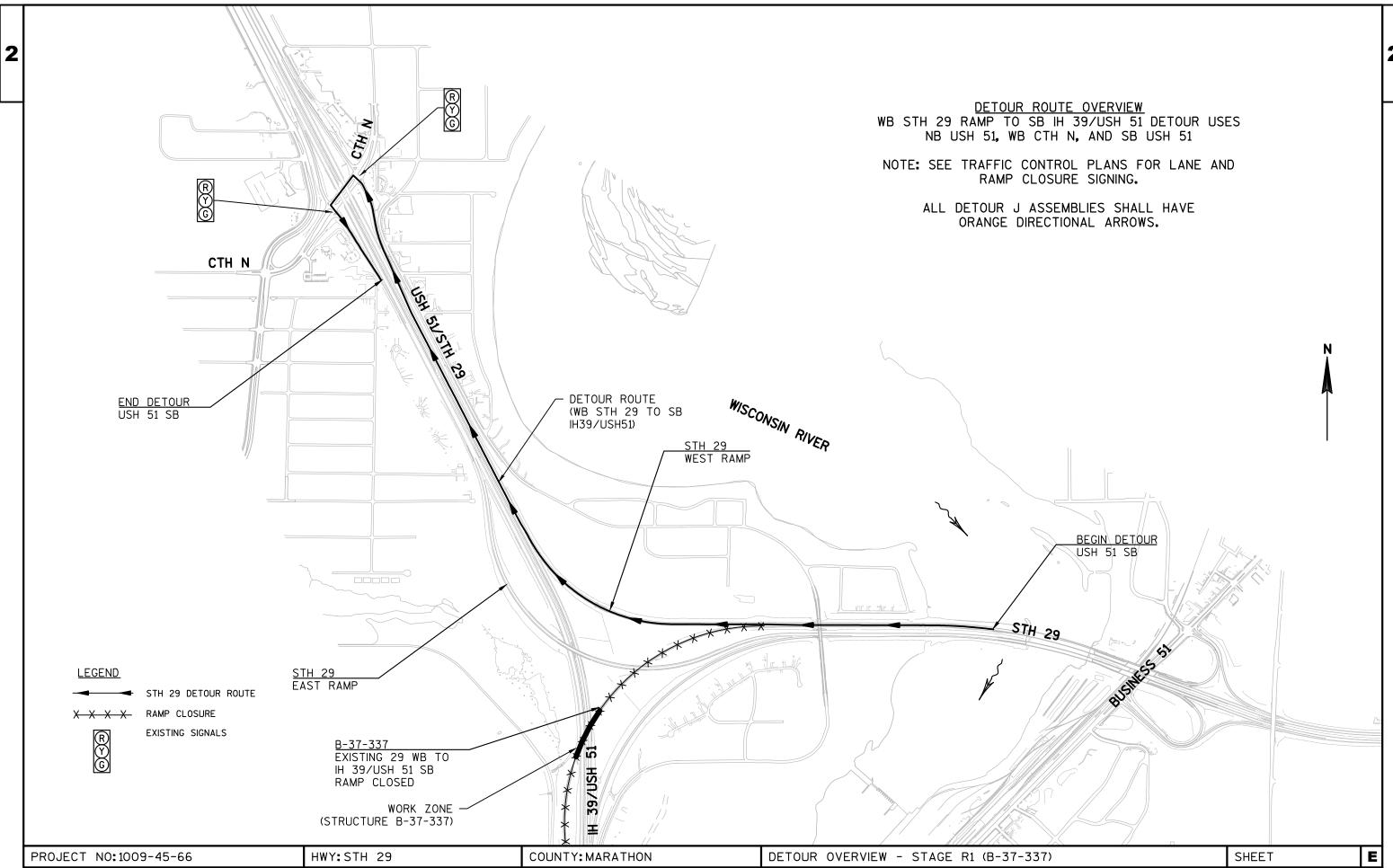


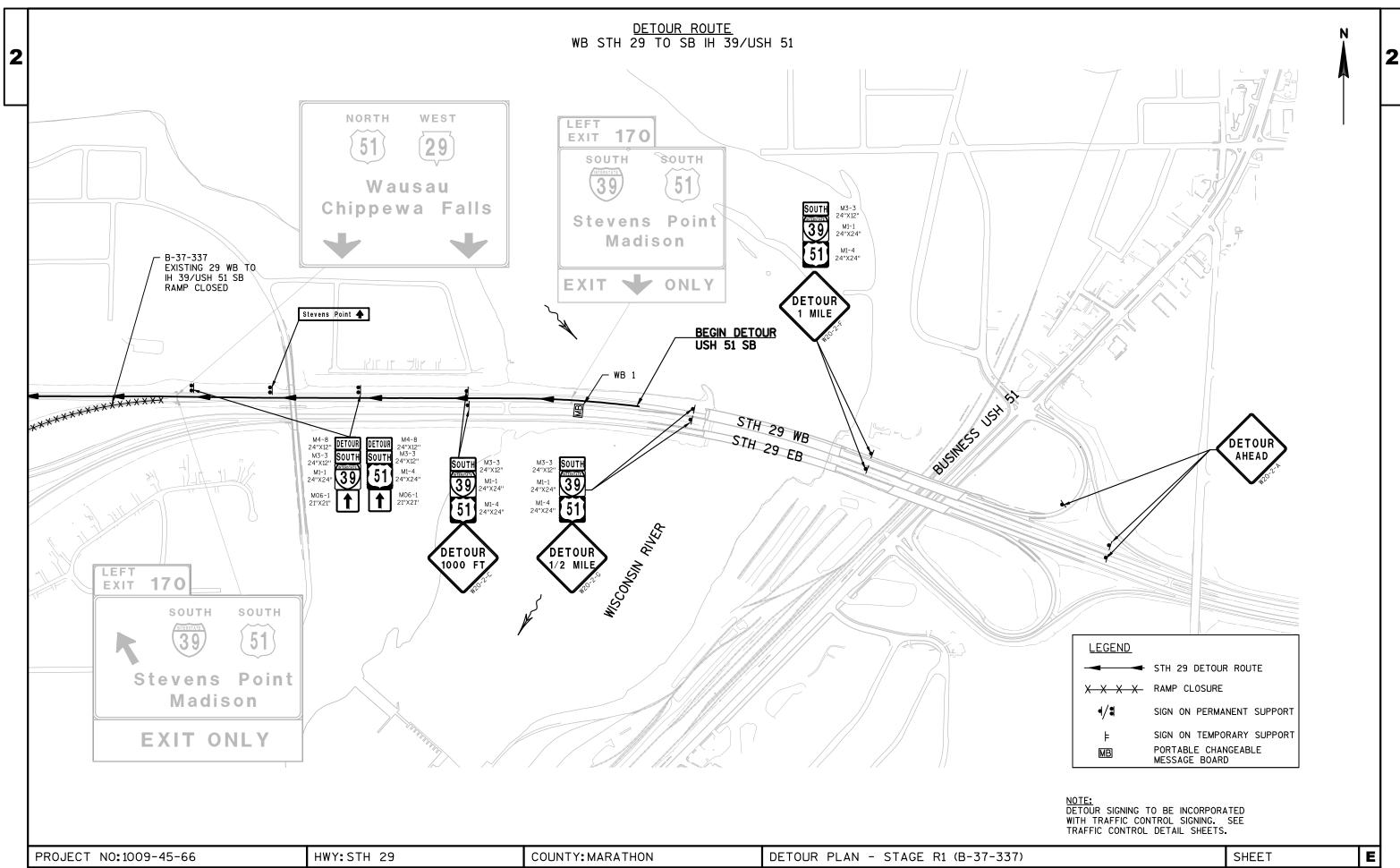


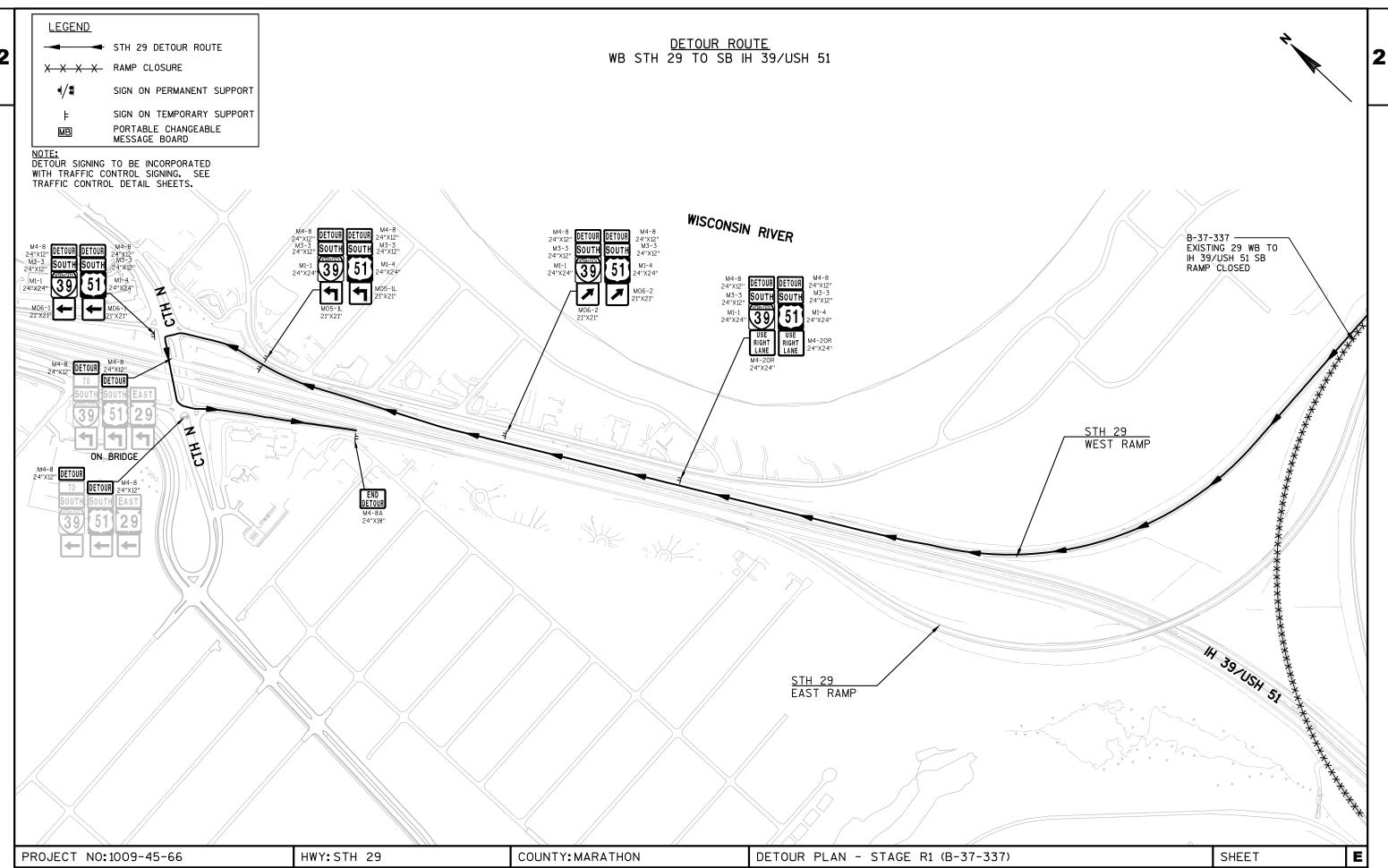


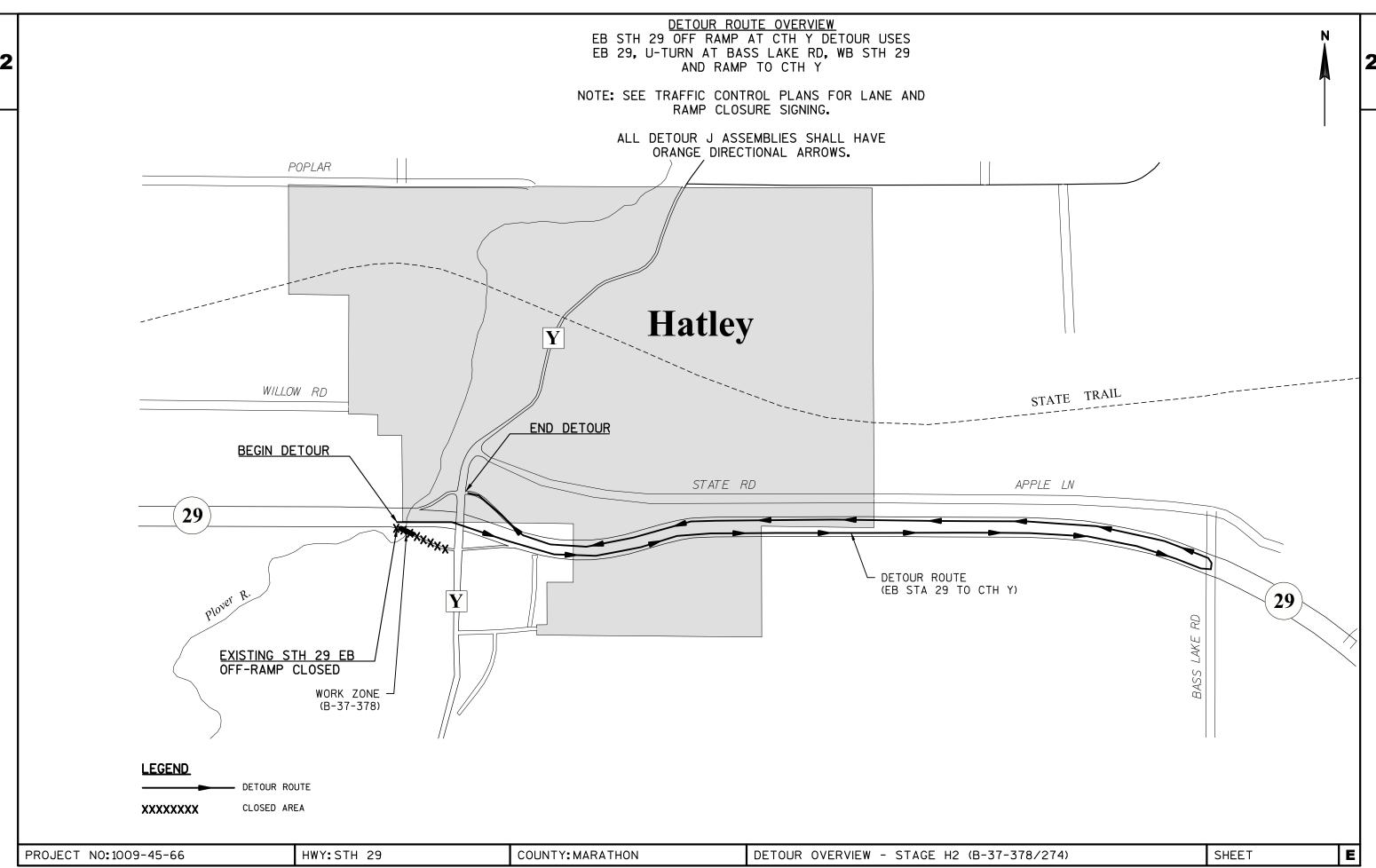


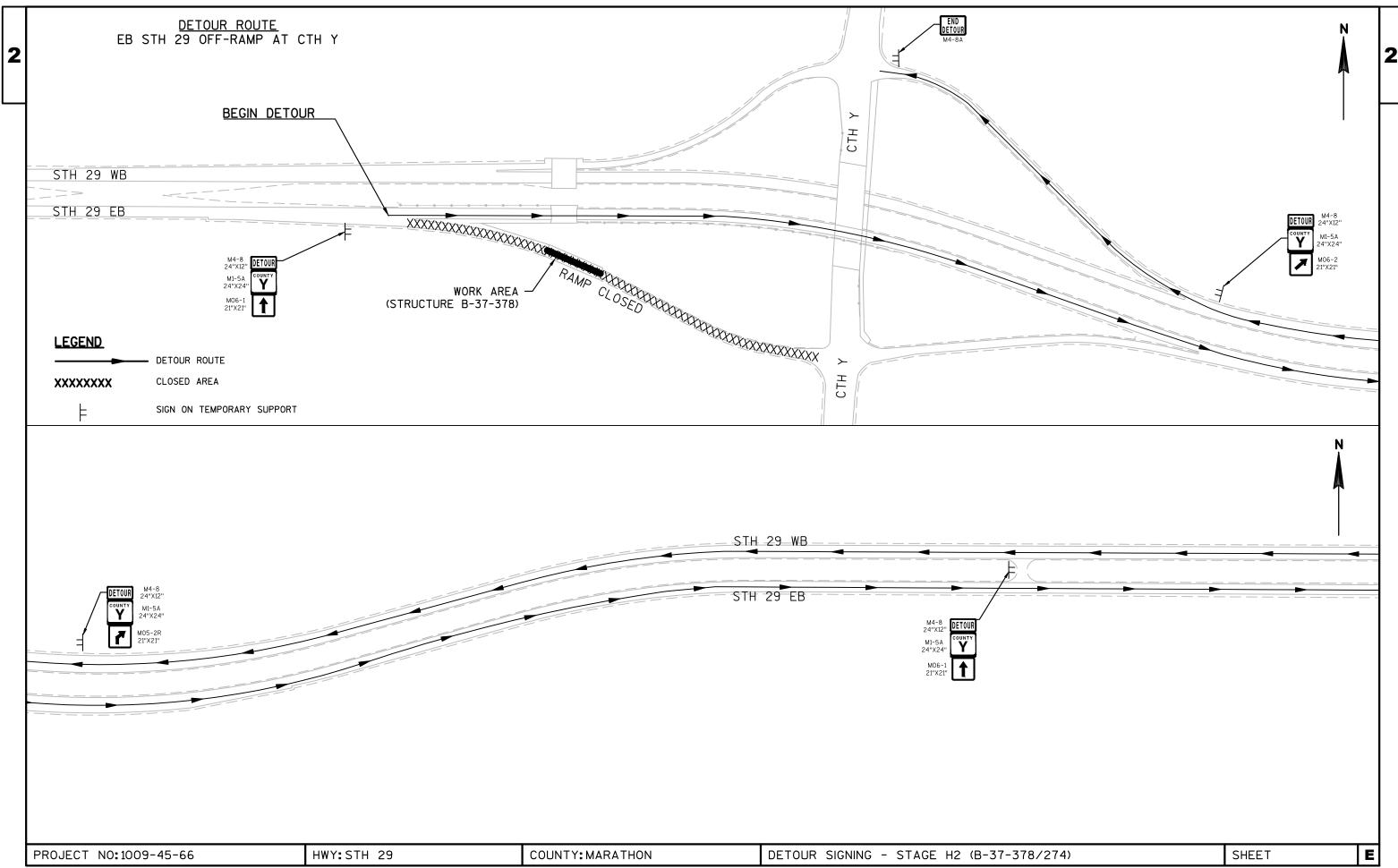


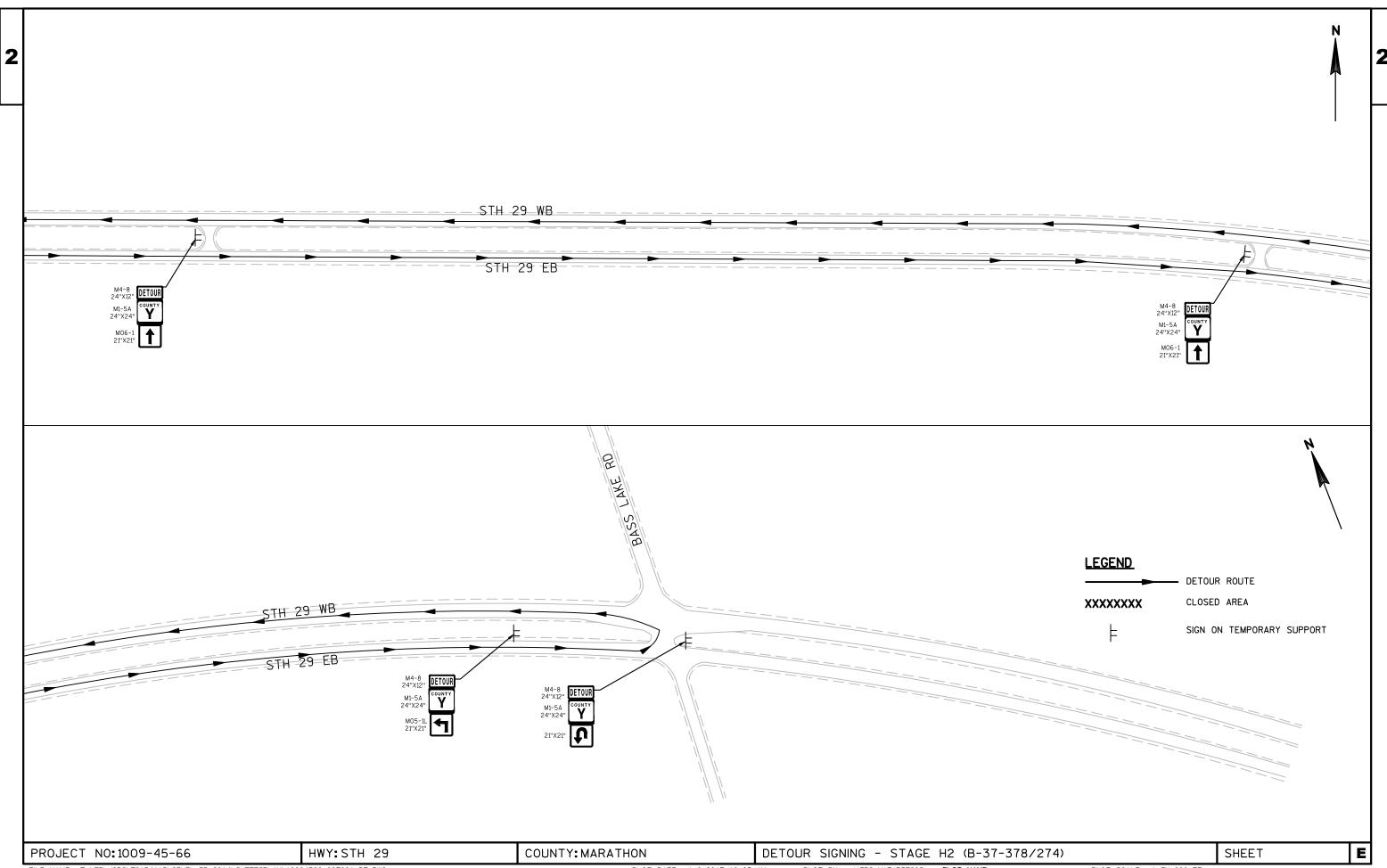


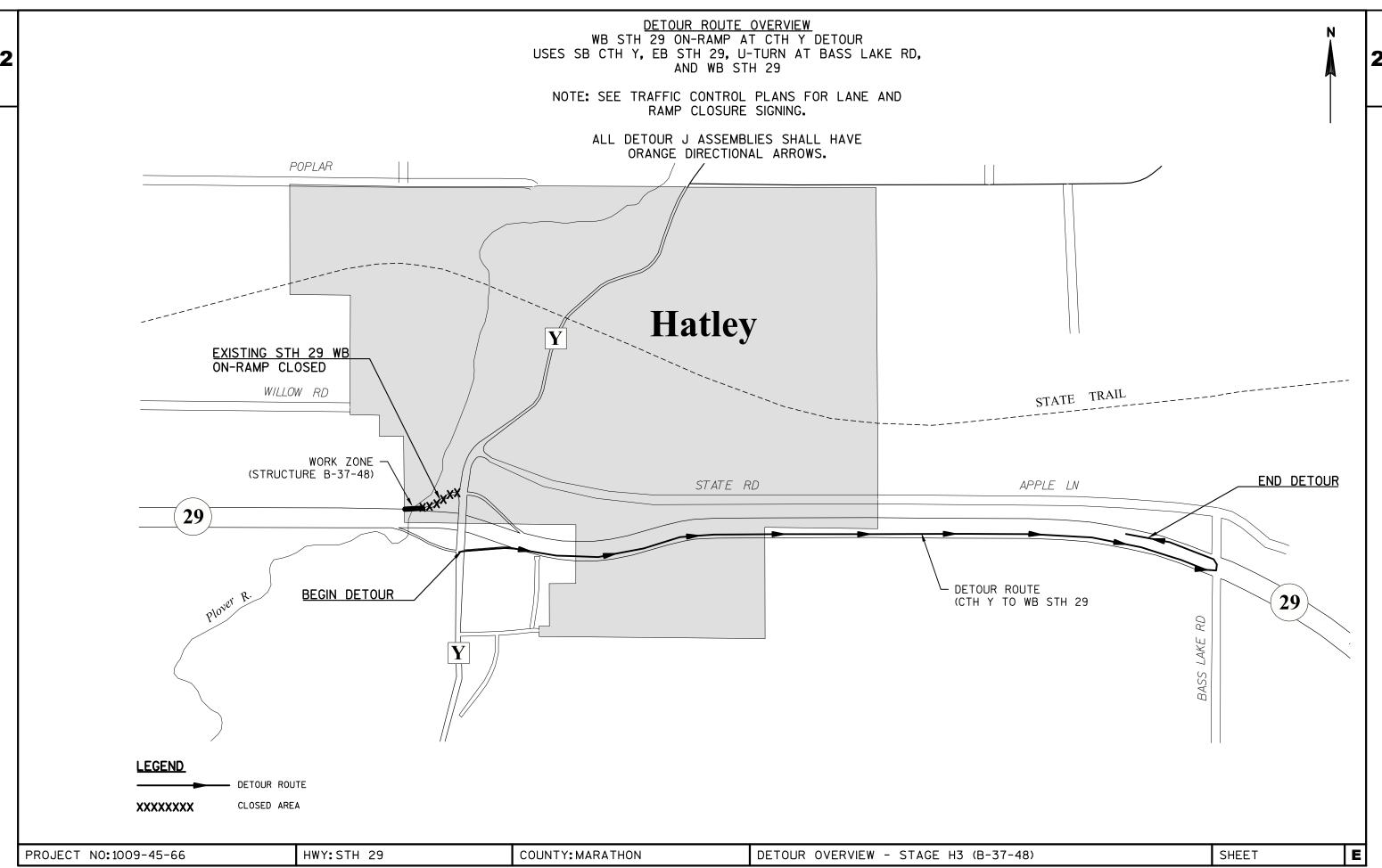


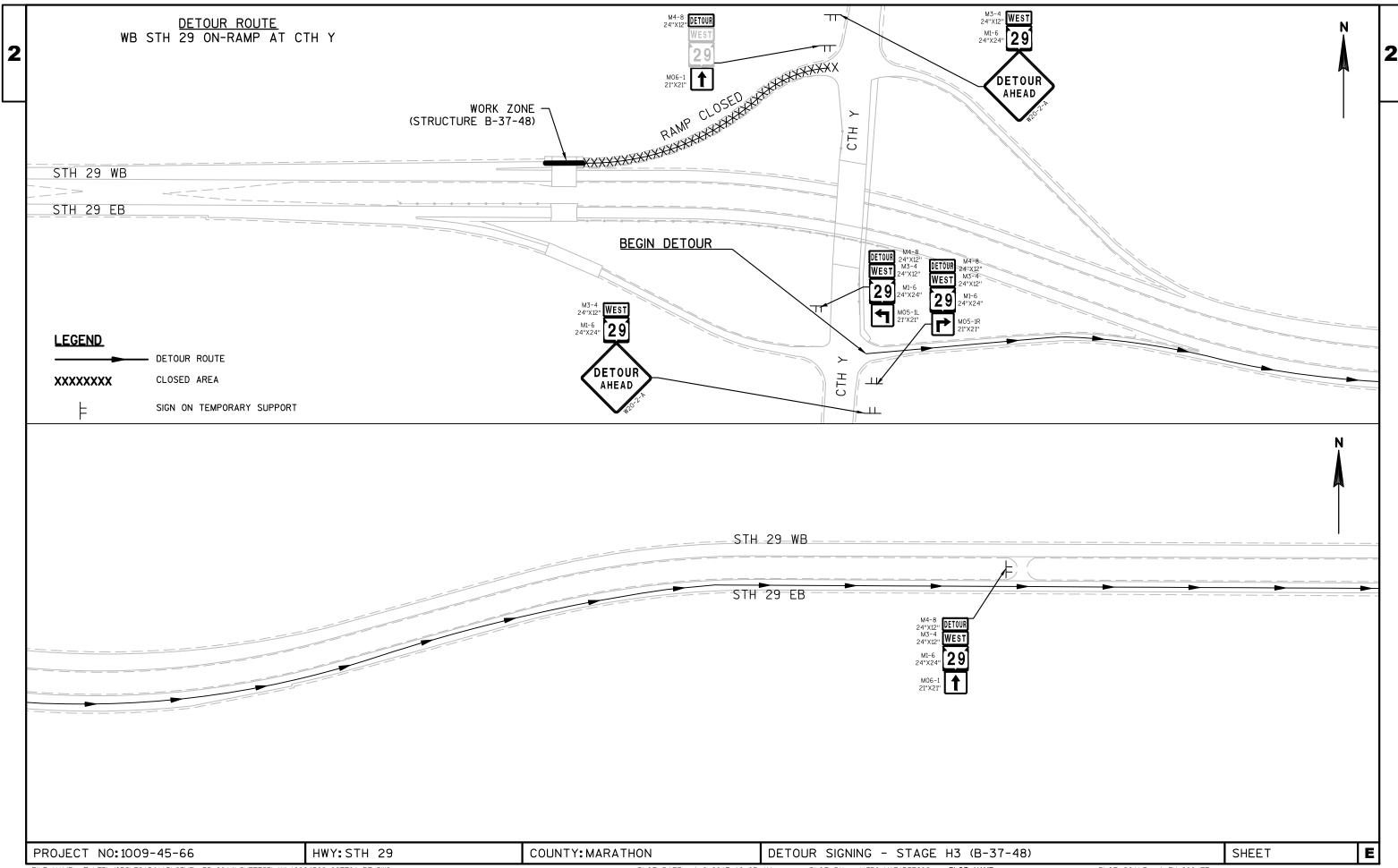


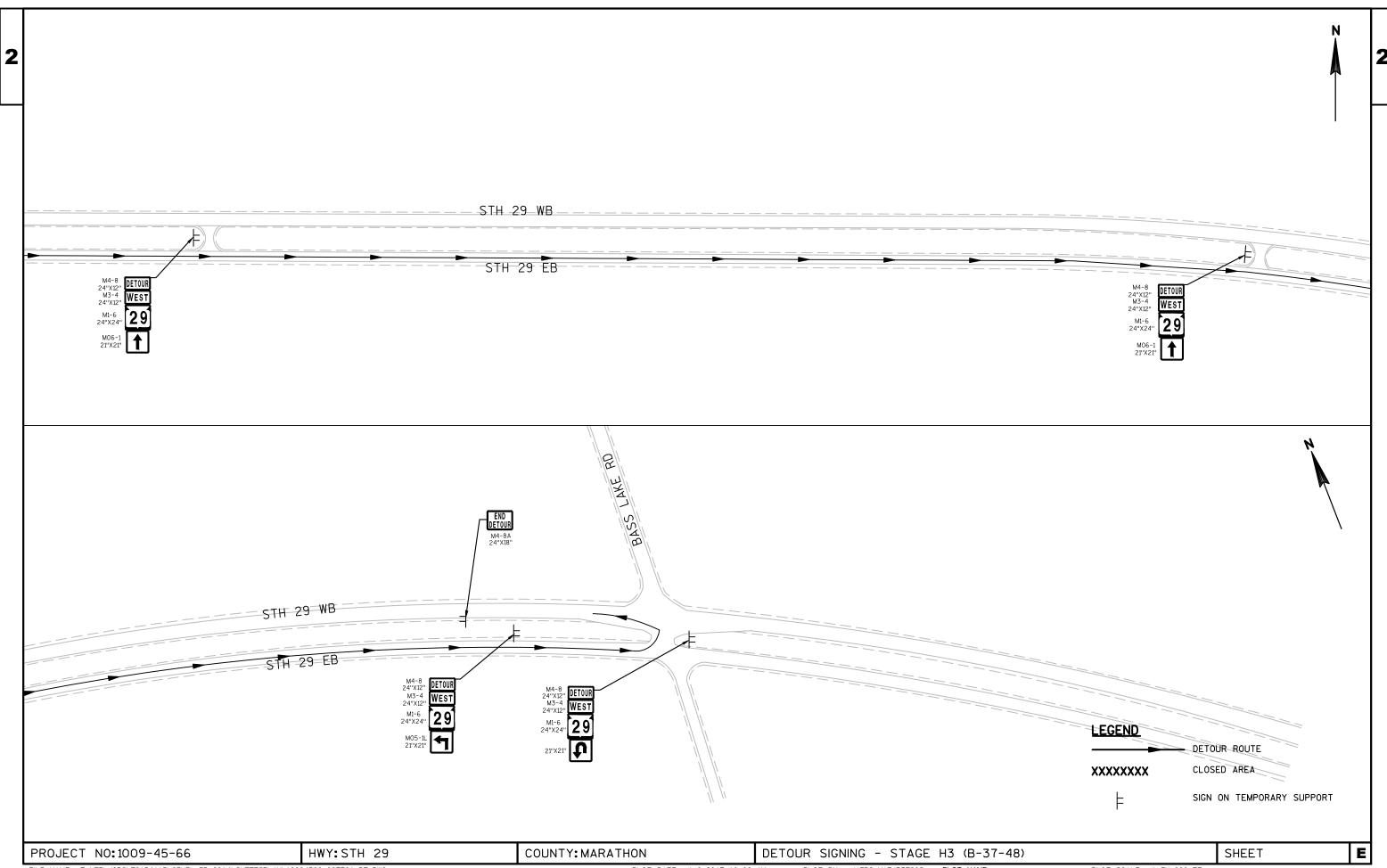












Estimate Of Quantities

					1009-45-66	
Line	Item	Item Description	Unit	Total	Qty	
0002	213.0100	Finishing Roadway (project) 01. 1009-45-66	EACH	1.000	1.000	
0004	509.5100.S	Polymer Overlay	SY	27,395.000	27,395.000	
0006	509.9025.S	Epoxy Injection Crack Repair	LF	100.000	100.000	
8000	509.9026.S	Cored Holes 2-Inch Diameter	EACH	4.000	4.000	
0010	517.1010.S	Concrete Staining (structure) 01. B-37-364	SF	1,700.000	1,700.000	
0012	619.1000	Mobilization	EACH	1.000	1.000	
0014	628.7010	Inlet Protection Type B	EACH	20.000	20.000	
0016	642.5001	Field Office Type B	EACH	1.000	1.000	
0018	643.0100	Traffic Control (project) 01. 1009-45-66	EACH	1.000	1.000	
0020	643.0300	Traffic Control Drums	DAY	8,895.000	8,895.000	
0022	643.0420	Traffic Control Barricades Type III	DAY	600.000	600.000	
0024	643.0705	Traffic Control Warning Lights Type A	DAY	1,200.000	1,200.000	
0026	643.0715	Traffic Control Warning Lights Type C	DAY	1,420.000	1,420.000	
0028	643.0800	Traffic Control Arrow Boards	DAY	135.000	135.000	
0030	643.0900	Traffic Control Signs	DAY	2,254.000	2,254.000	
0032	643.0910	Traffic Control Covering Signs Type I	EACH	6.000	6.000	
0034	643.0920	Traffic Control Covering Signs Type II	EACH	2.000	2.000	
0036	643.1050	Traffic Control Signs PCMS	DAY	176.000	176.000	
0038	643.2000	Traffic Control Detour (project) 01. 1009-45-66	EACH	1.000	1.000	
0040	643.3000	Traffic Control Detour Signs	DAY	1,425.000	1,425.000	
0042	646.0106	Pavement Marking Epoxy 4-Inch	LF	18,733.000	18,733.000	
0044	646.0126	Pavement Marking Epoxy 8-Inch	LF	655.000	655.000	
0046	647.0166	Pavement Marking Arrows Epoxy Type 2	EACH	3.000	3.000	
0048	647.0356	Pavement Marking Words Epoxy	EACH	3.000	3.000	
0050	649.0400	Temporary Pavement Marking Removable Tape 4-Inch	LF	16,730.000	16,730.000	
0052	649.0506	Temporary Pavement Marking Removable Mask-Out Tape 6-Inch	LF	3,075.000	3,075.000	
0054	649.0801	Temporary Pavement Marking Removable Tape 8-Inch	LF	600.000	600.000	
0056	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	1,200.000	1,200.000	
0058	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000	

COVERING SIGNS

		643.0910 TRAFFIC CONTROL COVERING SIGNS TYPE I				643.0920 TRAFFIC CONTROL COVERING SIGNS TYPE II				
LOCATION	STAGE	EACH	NUMBER OF CYCLES	NUMBER OF SIGNS	EACH	NUMBER OF CYCLES	NUMBER OF SIGNS			
CATEGORY 0010	•	•	•			•	•			
HATLEY	н2	1	1	1	0	0	0			
	W2	3	1	3	1	1	1			
WAUSAU	W3B	2	1	2	1	1	1			

PROJECT TOTALS 6

TEMPORARY PAVEMENT MARKING

			649.0400 TEMPORARY PAVEMENT MARKING REMOVABLE TAPE 4-INCH		649.0506	649.0801
					TEMPORARY PAVEMENT MARKING REMOVABLE MASK-OUT TAPE 6-INCH	TEMPORARY PAVEMENT MARKING REMOVABLE TAPE 8-INCH
		STRUCTURE	(WHITE)	(YELLOW)	(BLACK)	(WHITE)
LOCATION	STAGE	NUMBER	LF	LF	LF	LF
CATEGORY 0010						
	н1	в-37-048 в-37-274		1320	360	
HATLEY	н2	B-37-274 B-37-378	1260	1060	525	200
	Н3	в-37-048	910	860	360	400
RIB MOUNTAIN	R1	в-37-337				
	W1A	B-37-355 B-37-357	1560		390	
	W1B	в-37-359 в-37-361		1560	390	
	W2	в-37-362	3700			
WAUSAU	W3A	в-37-356 в-37-360	1680		420	
	W3B	B-37-364 B-37-370	1140		210	
	w3c	в-37-356		1680	420	
		SUBTOTALS	10250	6480	3075	600
	PROS	JECT TOTALS	167	730	3075	600

EROSION CONTROL

		628.7010
		INLET
		PROTECTION
	STRUCTURE	TYPE B
LOCATION	NUMBER	EACH
CATEGORY 0010		
	в-37-048	0
HATLEY	в-37-274	0
	в-37-378	0
RIB MOUNTAIN	в-37-337	4
	B-37-355	1
	в-37-356	1
	в-37-357	1
	в-37-359	2
	в-37-360-0001	1
WAUSAU	в-37-360-0002	0
WAUSAU	в-37-360-0003	2
	B-37-361	2
	в-37-362-0001	1
	в-37-362-0002	2
	в-37-364	1
	в-37-370	2

PROJECT TOTAL 20

PROJECT NO: 1009-45-66 HWY: STH 29 COUNTY: MARATHON MISCELLANEOUS QUANTITIES SHEET E 3.2

FILE NAME: F:\TR\JOBS\E2154A14\Quantity\10094566_030201_MQ.pptx ORIGINATOR: OMNNI ASSOCIATES ORIG. DATE:

PRINT DATE: September 15, 2017

TRAFFIC CONTROL

				643.0100	643.	0300	643	.0420	643.0	0705	643.	0715	643.	0800	643.	0900	643.	1050	643.	3000	
			EST.	TRAFFIC	DRU		BARR	CADES	WARN			NING		ROW		GNS		GNS		SIGNS	
			SERVICE	CONTROL			TYPE	III	LIG	HTS	LIG	HTS	BOA	ARDS			PC	MS			
STRUCTURE			PERIOD	(PROJECT)					TYPI	ΕA	TYP	E C									
NUMBER	STAGE	LOCATION	DAYS	EACH	NO	DAYS	NO	DAYS	NO	DAYS	NO	DAYS	NO	DAYS	NO	DAYS	NO	DAYS	NO	DAYS	REMARKS
PROJECT 1009-4	45-66 CA	ATEGORY 0010		1																	
		STH 29 EB	3		76	228	1	3	2	6	13	39	2	6	15	45	1	16	0		NOTE A
		STH 29 WB	3		108	324	3	9	6	18	13	39	2	6	19	57	1	14	0		NOTE A
	н1	STH 29 EB OFF RAMP	3		0		0		0		0		0		1	3	0		0		
	пт	STH 29 WB ON RAMP	3		0		0		0		0		0		1	3	0		0		
		CTH Y NB	3		0		0		0		0		0		0		1	16	0		NOTE B
		CTH Y SB	3		0		0		0		0		0		0		1	16	0		NOTE B
		STH 29 EB	3		208	624	6	18	12	36	33	99	2	6	24	72	1	2	18	54	
		STH 29 WB	3		30	90	0		0		0		0		4	12	1	2	7	21	
		STH 29 EB OFF RAMP	3		0		6	18	12	36	0		0		3	9	0		0		
		STH 29 EB ON RAMP	3		29	87	0		0		9	27	0		3	9	0		0		
HATLEY	н2	BASS LAKE ROAD	3		0		10	30	20	60	0		0		2	6	0		0		
B-37-274	112	STH 29 NEAR HILLTOP ROAD	3		0		0		0		0		0		0		1	2	0		
B-37-378		CTH Y NB	3		0		0		0		0		0		0		1	2	0		NOTE B
B-37-48		CTH Y SB	3		0		0		0		0		0		0		1	2	0		NOTE B
		STATE RD	3		0		3	9	6	18	0		0		5	15	0		0		
		BASS LAKE ROAD AT HILLTOP ROAD	3		0		0		0		0		0		3	9	0		0		
		STH 29 EB	5		99	495	1	5	2	10	18	90	2	10	18	90	1	4	20	100	
		STH 29 WB	5		82	410	2	10	4	20	19	95	2	10	20	100	1	4	1	5	
		STH 29 WB ON RAMP	5		0		3	15	6	30	0		0		1	5	0		0		
		BASS LAKE ROAD	5		0		10	50	20	100	0		0		2	10	0		0		
	Н3	STH 29 NEAR HILLTOP ROAD	5		0		0		0		0		0		0		1	4	0		
		CTH Y NB	5		0		0		0		0		0		0		1	4	7	35	NOTE B
		CTH Y SB	5		0		0		0		0		0		0		1	4	11	55	NOTE B
		STATE RD	5		0		3	15	6	30	0		0		5	25	0		0		
		BASS LAKE ROAD AT HILLTOP ROAD	5		0		0		0		0		0		3	15	0		0		
RIB MOUNTAIN		STH 29 WB	3		80	240	7	21	14	42	0		0		11	33	1	16	0		NOTE C
в-37-337	R1	STH 29 WB - DETOUR	3		0		0		0		0		0		0		0		60	180	
		CTH N INTERCHANGE - DETOUR	3		0		0		0		0		0		0		0		21	63	
WAUSAU AREA	W1A	STH 29 EB OUTSIDE LANE	10		61	610	3	30	6	60	16	160	1	10	15	150	1	14	0		NOTE A
B-37-355 B-37-356	W1B	STH 29 EB INSIDE LANE	10		76	760	3	30	6	60	16	160	2	20	15	150	0		0		
B-37-357		EB STH 29 RAMP TO NB USH 51	15		80	1,200	6	90	12	180	0		0		17	255	1	25	31	465	NOTE D
B-37-359	W2	STH 52 ON RAMP TO SB USH 51																			NOTE D
в-37-360			10		12	120	2	20	4	40	0		0		/	70	0		0		
B-37-361	W3A	STH 29 WB OUTSIDE LANE	10		69	690	3	30	6	60	17	170	2	20	23	230	1	14	0		NOTE A
B-37-362	w3B	STH 29 WB OUTSIDE LANE;																			
в-37-364	VV JO	USH51 SB RAMP TO STH 29 WB	5		114	570	7	35	14	70	17	85	2	10	38	190	1	15	32	160	NOTE D
в-37-370	w3C	STH 29 WB INSIDE LANE	10		69	690	4	40	8	80	17	170	1	10	24	240			0		
		UNDIS	STRIBUTED			1,757		122		244		286		27		451		0		287	
			CT TOTALS	1		8,895		600		1,200		1,420		135		2,254		176		1,425	

NOTE A: INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) TWO WEEKS IN ADVANCE OF INITIAL LANE CLOSURE ON STH 29.

NOTE B: INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) AT RAMP LOCATIONS ON CTH Y TWO WEEKS IN ADVANCE OF RAMP CLOSURES.

NOTE C: INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) TWO WEEKS IN ADVANCE OF INITIAL RAMP CLOSURE ON STH 29 APPROXIMATELY ONE HALF MILE IN ADVANCE OF PROJECT.

NOTE D: INSTALL PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) TWO WEEKS IN ADVANCE OF INITIAL RAMP CLOSURE ON RAMP

PROJECT NO: 1009-45-66 HWY: STH 29 COUNTY: MARATHON MISCELLANEOUS QUANTITIES SHEET E 3

3

PAVEMENT MARKING

					646.0106	646.0106	646.0126	647.0166	647.0356	
					PAVEMENT MARKING EPOXY	PAVEMENT MARKING EPOXY	PAVEMENT MARKING EPOXY	PAVEMENT MARKING ARROWS EPOXY	PAVEMENT MARKING WORDS EPOXY	DEMARKS
					4-INCH	4-INCH	8-INCH	TYPE 2	"ONLY"	REMARKS
	STRUCTURE	FROM		TO	(WHITE)	(YELLOW)	(WHITE)	(WHITE)	(WHITE)	
LOCATION	NUMBER	(STATION)		(STATION)	LF	LF	LF	EACH	EACH	
CATEGORY 0010										
HATLEY	B-37-048	827+60	-	828+18	211	65				
HATLEY	B-37-274	827+57	-	828+17	84	68				
HATLEY	B-37-378	12+71	-	13+69	104	104				
RIB MOUNTAIN	в-37-337	51+77	-	57+83	612	612				
WAUSAU	в-37-355	3105+02	-	3107+01	237	190				
WAUSAU	в-37-356	4105+03	ı	4106+87	237	190				
WAUSAU	в-37-357	3110+37	ı	3112+17	234	187				
WAUSAU	в-37-359	3115+66	ı	3117+95	294	235				
WAUSAU	в-37-360	4111+67	1	4131+63	783	626				
WAUSAU	B-37-361	3090+24	-	3093+07	256	205				
WAUSAU	B-37-362	605+56	-	633+57	2096	2096				
WAUSAU	в-37-364	645+23	-	648+14	297	297				
WAUSAU	в-37-370	4091+81.02	-	4093+78	308	205				
WAUSAU; DETOUR	в-37-362	N. SIDE		USH 51 ON-						
ON STH 52 EB	B-37-302	STEWART AVE.	-	RAMP TERMINI	4400					NOTE A
WAUSAU; DETOUR	в-37-362	N. SIDE		USH 51 ON-						
ION STH 52 FR I		STEWART AVE.	1	RAMP TERMINI			550	3	3	NOTE B
WAUSAU; DETOUR	D 27 264	USH 51 OFF-		N. SIDE						
ON STH 52 WB	B-37-364	RAMP TERMINI	-	STEWART AVE.	3500					NOTE A
WAUSAU; DETOUR	D 27 264	USH 51 OFF-		N. SIDE						
ON STH 52 WB	B-3/-304	RAMP TERMINI	_	STEWART AVE.			105			NOTE C
		PI	ROJ	ECT SUBTOTALS	13653	5,080	655	3	3	
			ı	PROJECT TOTAL	. 18,	733	655	3	3	

NOTE A: LANE LINES AND RIGHT EDGE LINES (NO CHANNELIZING LINES).

NOTE B: CHANNELIZING LINE AT USH 51 ON-RAMP BETWEEN LEFT TURN AND THROUGH LANES.

NOTE C: CHANNELIZING LINE AT STEWART AVE. BETWEEN LEFT TURN AND THROUGH LANES.

PROJECT NO: 1009-45-66 HWY: STH 29 COUNTY: MARATHON MISCELLANEOUS QUANTITIES SHEET E 3.2

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-03	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C07-14B	PAVEMENT MARKING WORDS
15C07-14C	PAVEMENT MARKING ARROWS
15C08-18A	LONGITUDINAL MARKING (MAINLINE)
15C12-06	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15D12-06A	TRAFFIC CONTROL, LANE CLOSURE
15D12-06B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION
15D15-03D	TRAFFIC CONTROL, TAPERED ENTRANCE RAMP WITHIN LANE CLOSURE
15D15-03E	TRAFFIC CONTROL, PARALLEL EXIT RAMP WITHIN LANE CLOSURE
15D16-03	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D38-02A	TEMPORARY TRAFFIC CONTROL SIGN MOUNTING
15D38-02B	ATTACHMENT OF SIGNS TO POSTS





INLET PROTECTION, TYPE A

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- 1) FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- (2) FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- (3) FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE, THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION TYPE A, B, C, AND D

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02

/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER 6

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ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E LANE CLOSURE BARRICADE DETAIL APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30". R11-3, R11-4 AND R10-61 SHALL BE 60" X 30". M4-9 SHALL BE 30" X 24". M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.) M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.) M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.) MO5-1 AND MO6-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.) D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS. R1-1 SHALL BE 36" X 36".

- (1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT
- THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS. PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

2

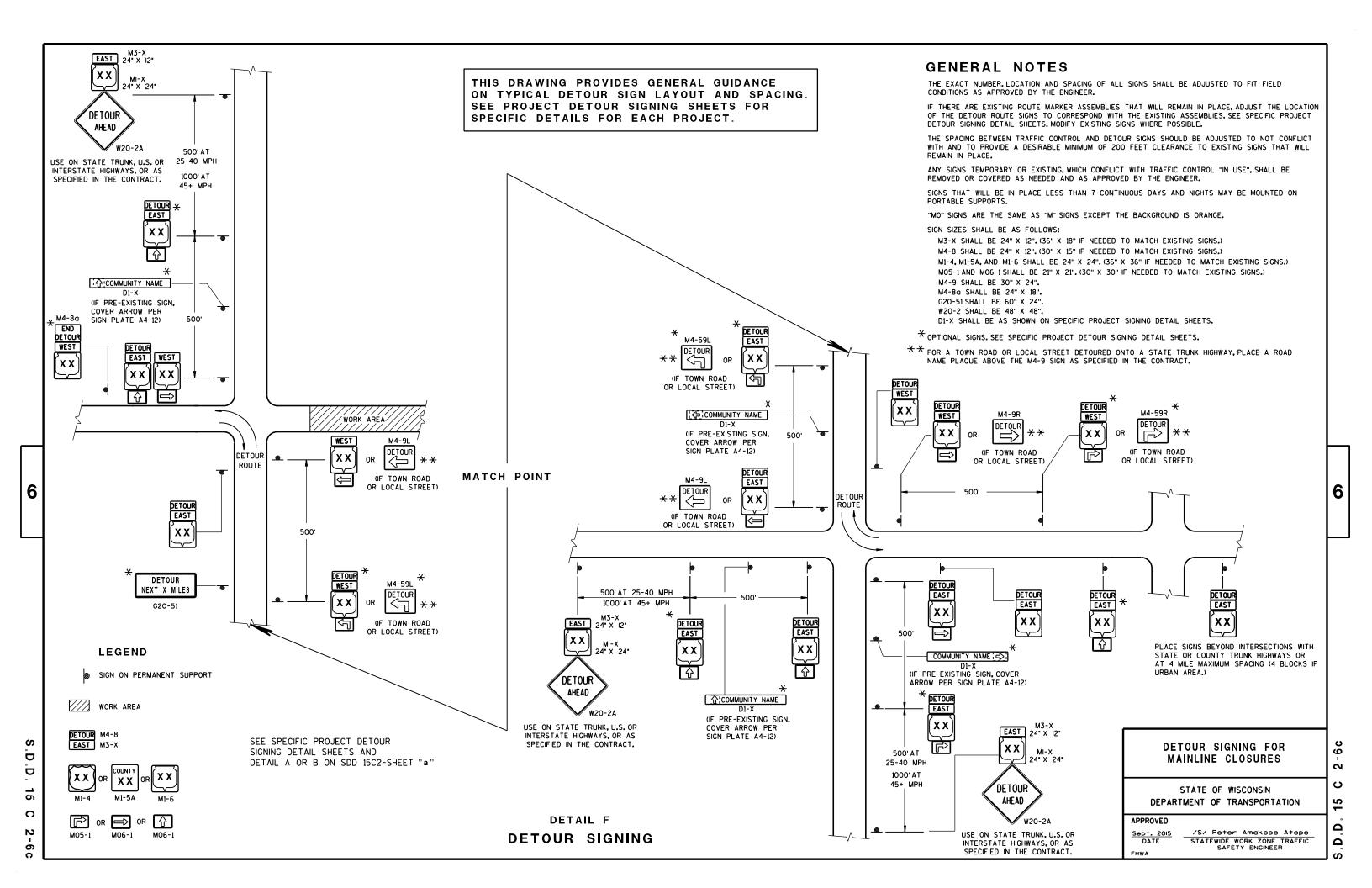
2

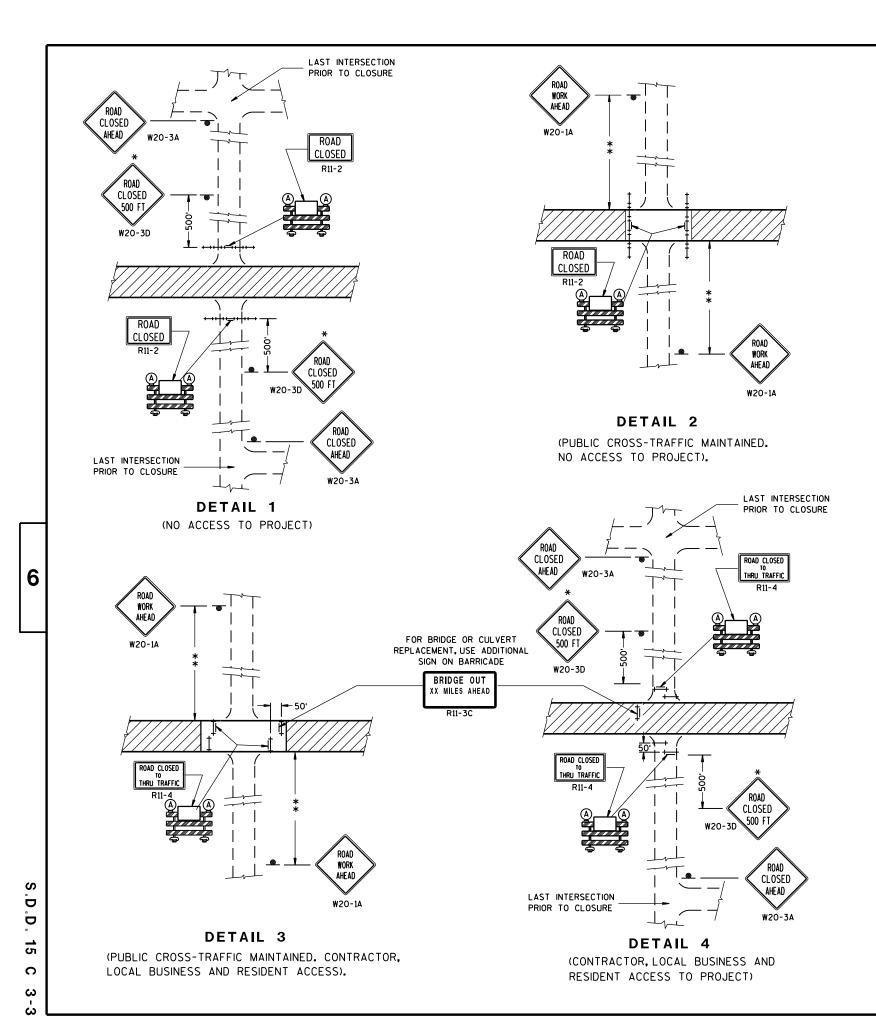
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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

/S/ Peter Amakobe Atepe

STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER





GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

SIGN ON PERMANENT SUPPORT

TYPE III BARRICADE

TYPE III BARRICADE WITH
ATTACHED SIGN

(A) TYPE "A" WARNING LIGHT (FLASHING)

WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

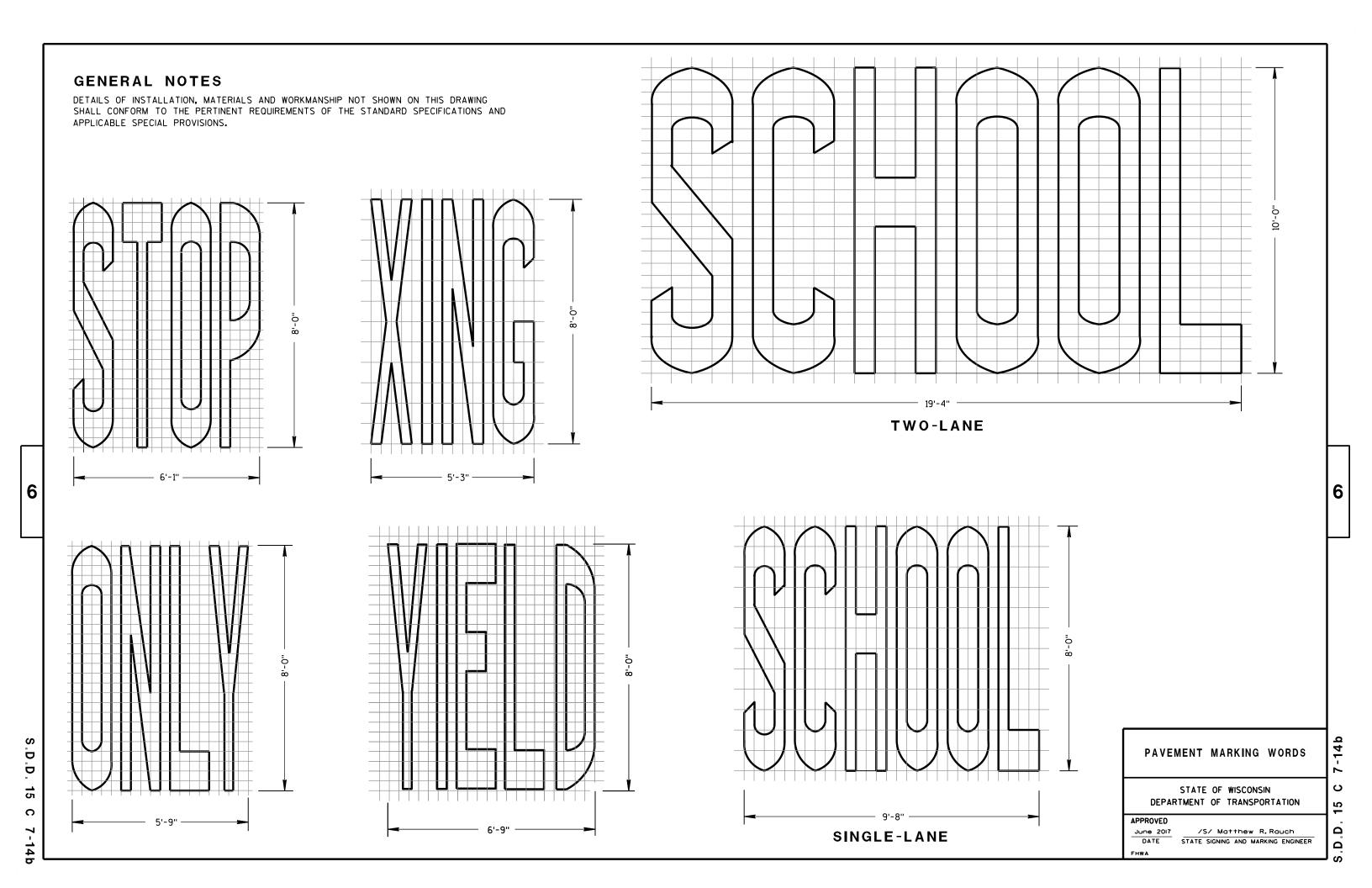
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

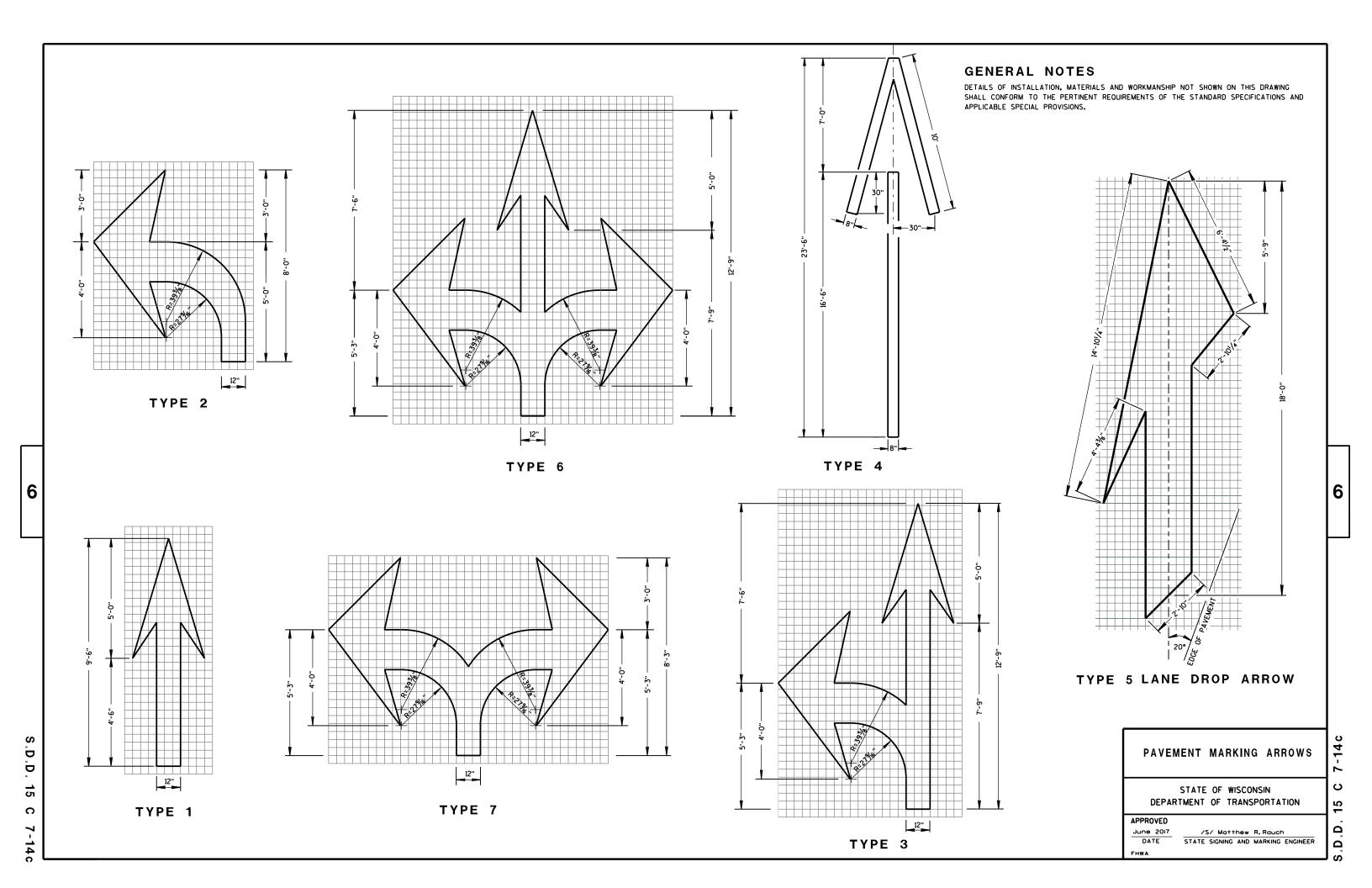
APPROVED

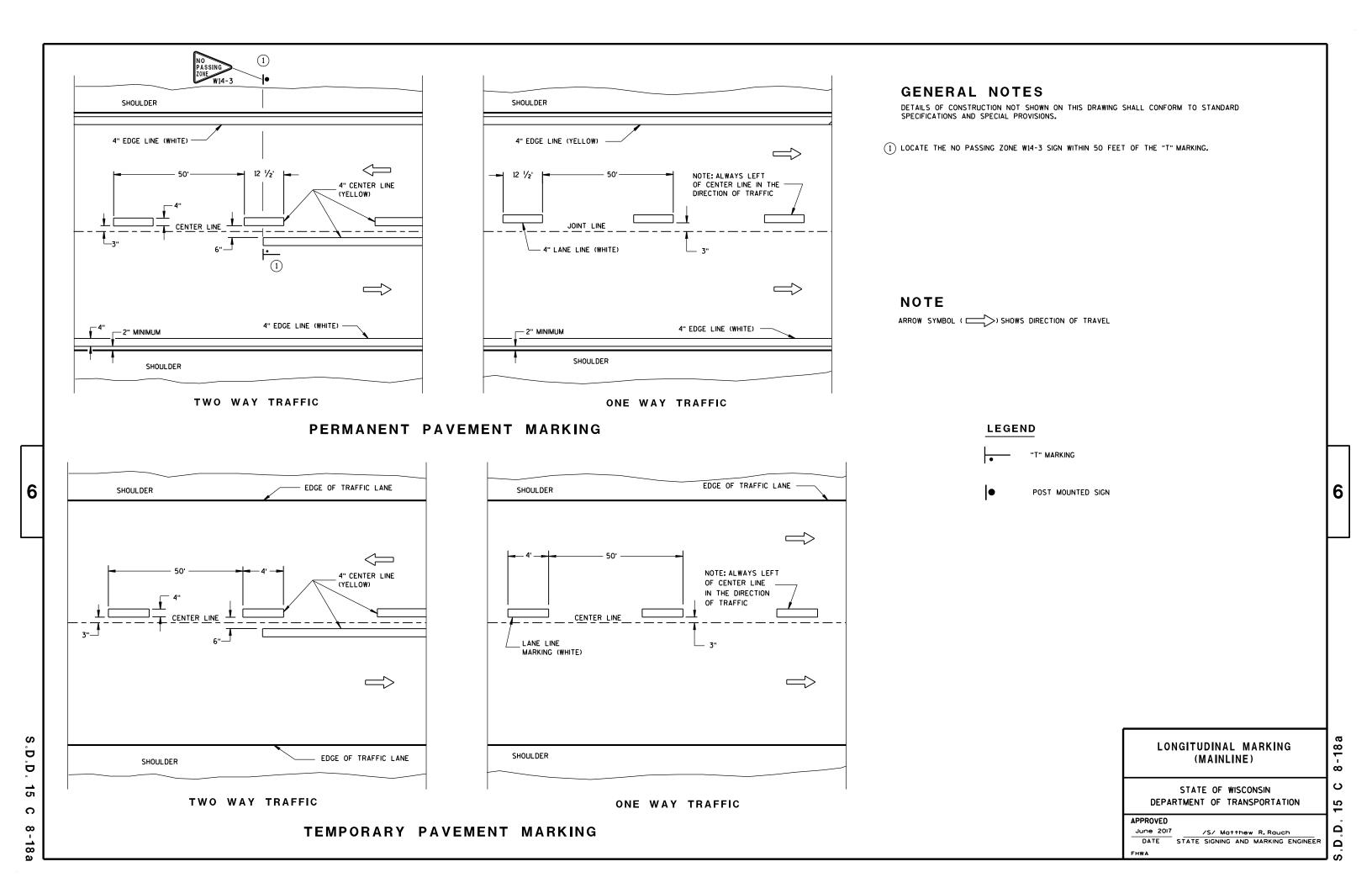
Sept. 2015

DATE
STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

S.D.D. 15 C 3







TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STOP/SLOW PADDLE ON SUPPORT STAFF

5' MIN.

WORK

AHEAD

48" X 24"

END ROAD WORK G20-2A

(2)

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W20-1A

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS. PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT. REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* UTILIZE TEMPORARY RUMBLE STRIPS WHEN FLAGGING OPERATION IS ANTICIPATED TO BE STATIONARY IN EXCESS OF TWO HOURS.

- 1) FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS (IF USED) SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- EACH TEMPORARY RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

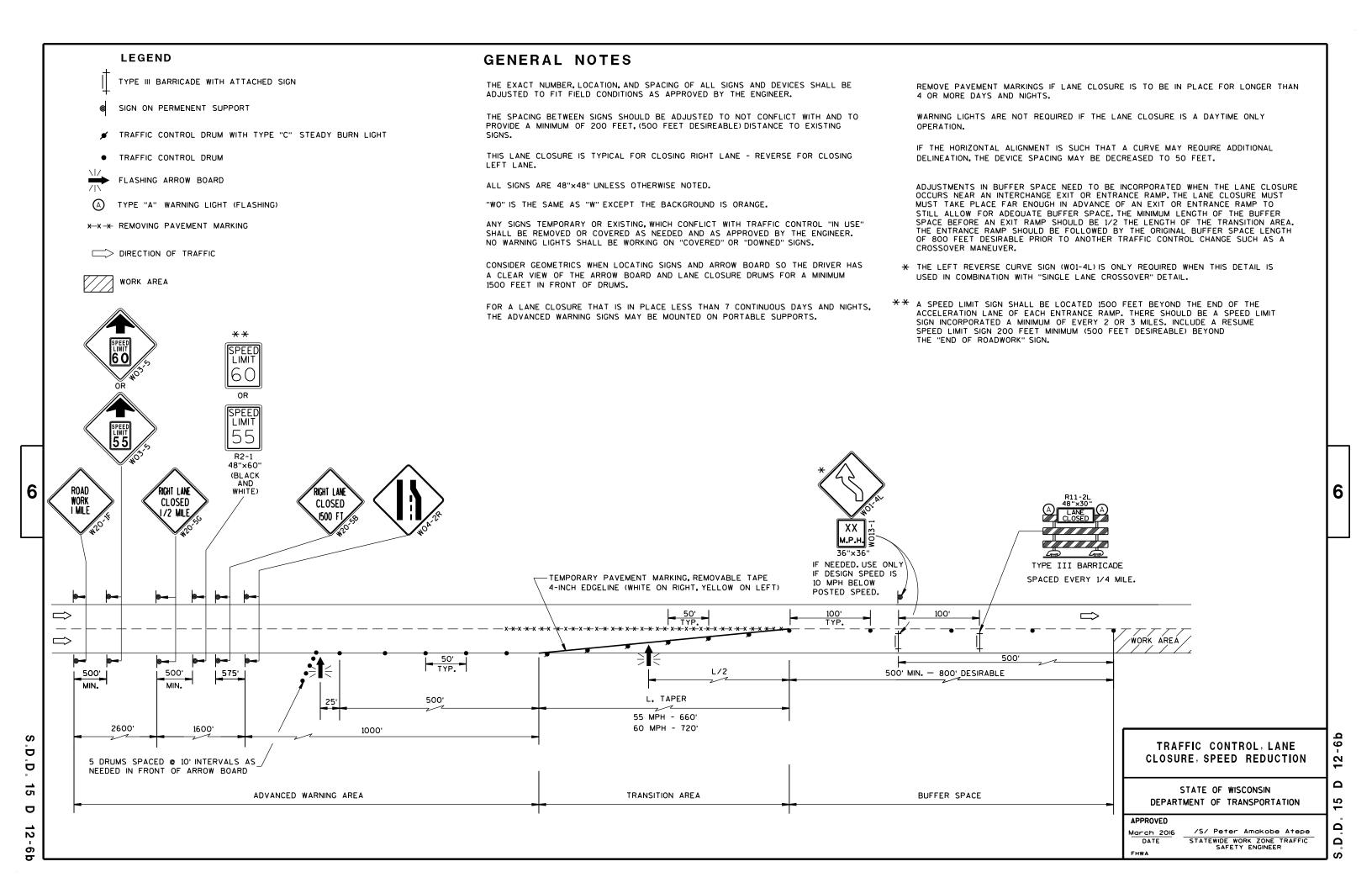
TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

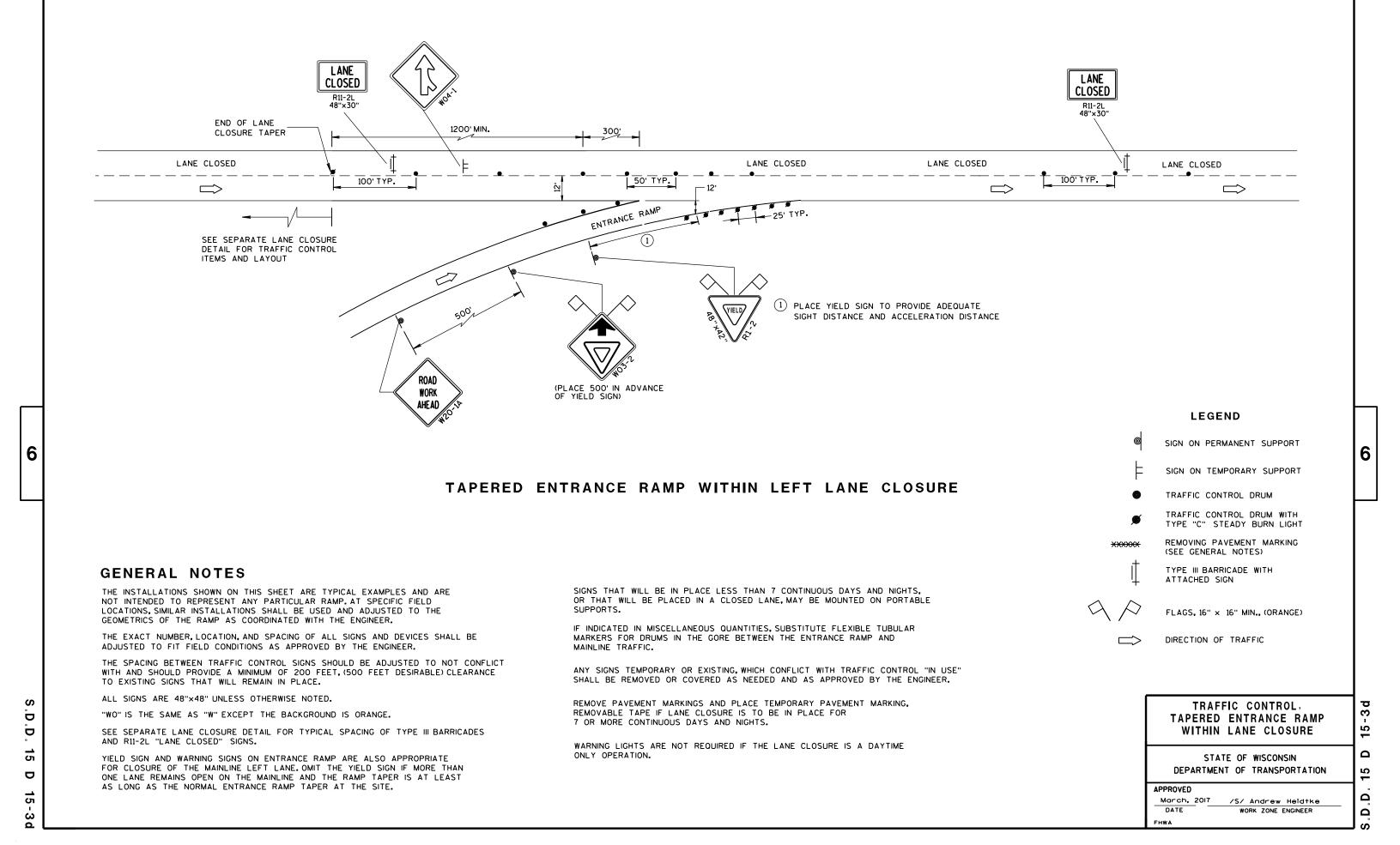
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED	
June 2017	/S/ Andrew Heidtke
DATE	WORK ZONE ENGINEER
FHWA	

D Ö 15 C 2

GENERAL NOTES LEGEND THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR LONGER THAN ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER. 4 OR MORE DAYS AND NIGHTS. TYPE III BARRICADE WITH ATTACHED SIGN THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION. SIGN ON PERMENENT SUPPORT IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING DELINEATION. THE DEVICE SPACING MAY BE DECREASED TO 50 FEET. LEFT LANE. TRAFFIC CONTROL DRUM ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST FLASHING ARROW BOARD "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE. MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" TYPE "A" WARNING LIGHT (FLASHING) THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS. * X -X REMOVING PAVEMENT MARKING CROSSOVER MANEUVER. CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS * THE LEFT REVERSE CURVE SIGN (WO1-4L) IS ONLY REQUIRED WHEN THIS DETAIL IS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM USED IN COMBINATION WITH "SINGLE LANE CROSSOVER" DETAIL. DIRECTION OF TRAFFIC 1500 FEET IN FRONT OF DRUMS. FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS. THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS. 6 6 WORK CLOSED CLOSED I MILE 1500 F XX м.Р.н 36"×36" IF NEEDED. USE ONLY TYPE III BARRICADE IF DESIGN SPEED IS TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE SPACED EVERY 1/4 MILE. 10 MPH BELOW 4-INCH EDGELINE (WHITE ON RIGHT, YELLOW ON LEFT) POSTED SPEED. 100' \Rightarrow \Rightarrow \Longrightarrow WORK AREA 50' L/2 500' MIN. - 800' DESIRABLE 575 L. TAPER 500 50 MPH - 600' 55 MPH - 660' 2600' 1600' 1000' 60 MPH - 720' TRAFFIC CONTROL, 9 65 MPH - 780' D 70 MPH - 840' LANE CLOSURE 5 DRUMS SPACED @ 10' INTERVALS AS 2 Ö NEEDED IN FRONT OF ARROW BOARD 15 Ω STATE OF WISCONSIN ADVANCED WARNING AREA TRANSITION AREA BUFFER SPACE DEPARTMENT OF TRANSPORTATION D **APPROVED** /S/ Peter Amakobe Atepe 2 March 2016 STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER Ω 6 FHWA

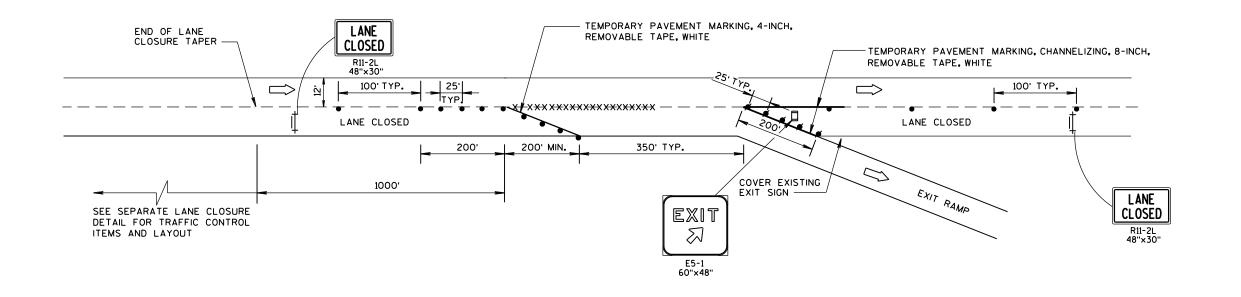






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PARALLEL EXIT RAMP

GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2L "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE.OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 7 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

LEGEND

SIGN ON PERMANENT SUPPORT

SIGN ON TEMPORARY SUPPORT

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT

REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)

TYPE III BARRICADE WITH ATTACHED SIGN

FLAGS, 16" × 16" MIN., (ORANGE)

DIRECTION OF TRAFFIC

TRAFFIC CONTROL, PARALLEL EXIT RAMP WITHIN LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED March, 2017	/S/ Andrew Heidtke
DATE	WORK ZONE ENGINEER
FHWA	

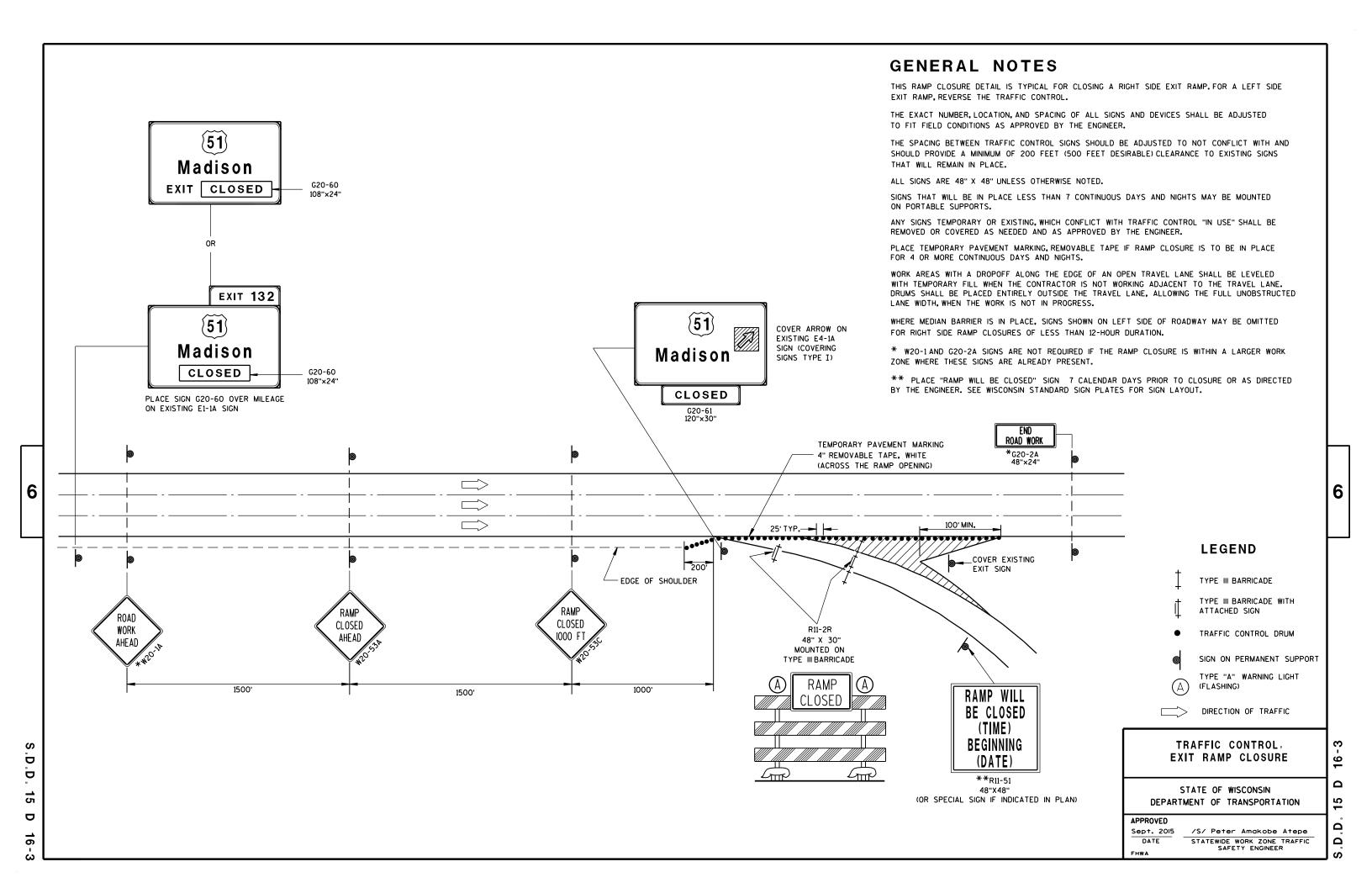
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TUBULAR STEEL POSTS

AREA OF SIGN INSTALLATION (SO. FT.)	NUMBER OF REQUIRED TUBULAR STEEL POSTS
9 OR LESS	1
GREATER THAN 9 LESS THAN OR EQUAL TO 18	2
GREATER THAN 18 LESS THAN OR EQUAL TO 27	3

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SO.FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE). SIGNS LARGER THAN 27 SO.FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

URBAN AREA

POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

WOOD POST **EMBEDMENT DEPTH**

AREA OF SIGN INSTALLATION (SQ. FT.)	D (MIN)
20 OR LESS	4'
GREATER THAN 20	5'

4" X 6" WOOD POST

POST SPACING REQUIREM	NUMBER OF		
L	E	WOOD POSTS REQUIRED	
48" OR LESS AND LESS THAN 20 SO.FT.	-	1	
LESS THAN 60"	12"	2	٤
60" TO 120"	L/5	2	
GREATER THAN 120" LESS THAN 168"	12"	3	
168" AND GREATER	12"	4	

SEE NOTE (3)

RURAL AREA

TEMPORARY TRAFFIC CONTROL SIGN MOUNTING

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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- 11/2" DIAMETER HOLES

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NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

- A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3
- B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3"

MACHINE BOLTS - 1/6" X 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS

RIVETS - 1/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

> ATTACHMENT OF SIGNS TO POSTS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

June 2017 /S/ Andrew Heidtke DATE WORK ZONE ENGINEER FHWA

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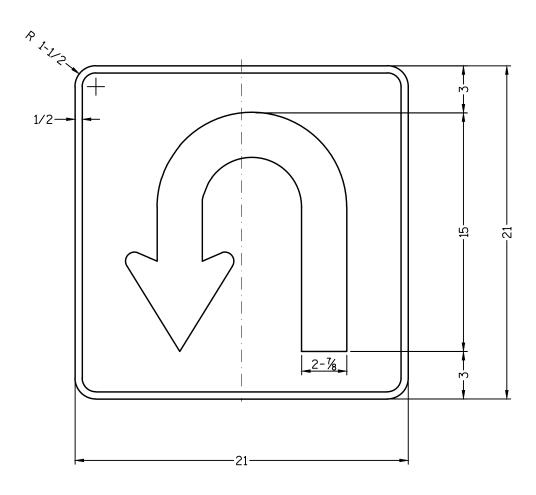
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38-2b

NOTES

- 1. Sign Is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background — ORANGE Message — BLACK



PROJECT NO:1009-45-66 HWY:STH 29 COUNTY:MARATHON SIGN DETAILS SHEET E

1009-45-66

TRAFFIC DATA

ADT = 17,700 (2042) RDS = 70 MPH

ESTIMATE OF QUANTITIES

509.5100.S POLYMER OVERLAY 475 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

DESIGN DATA

LIVE LOAD:

10/24/16

DESIGN LOADING: HS-20
INVENTORY RATING: HS-25
OPERATING RATING: HS-43
WISCONSIN STANDARD PERMIT
VEHICLE (Wis-SPV): 250 KIPS

NO. DATE

BRIDGE OFFICE CONTACT

WILLIAM DREHER (608) 266-8489

CONSULTANT CONTACT

KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900

REVISION

VARIES (65'-0" MIN.) - OVERLAY (POLYMER) LIMITS STAGE H3 CONST. STAGE HI CONST. 8'-0" 15'-0" VARIES 12'-0" 12'-0" 1'-6" 10'-2" 1'-6" SHOULDER LANE LANE LANE SHOULDER R STH 29 RAMP-- R STH 29 WB LONGIT. -EXIST. SLOPED FACE POLYMER CONST. JOINT -- 1/4" MIN. PARAPET "LF", TYP. - CROWN OVERLAY 0.015% 0.015% OVERLAY EDGE OF DECK (TYP.) CROSS SECTION THRU ROADWAY (LOOKING EAST)

59'-0" BACK TO BACK OF ABUTMENTS

56'-0"

28'-0"

SPAN 2

828+00

← € PIER

PLAN

2 SPAN FLAT SLAB

€ BRG.

E. ABUT.

8" PAVING NOTCH, TYP.

END OF SLAB

STA. 12NW+79.42

−R STH 29 WB

END OF SLAB

STA. 828+17.52

-R STH 29 WB RAMP

13NW+00

28'-0"

SPAN 1

-CROWN LINE

-EXIST. SLOPED FACE |

PARAPET "LF", TYP.

-€ BRG.

W. ABUT.

ニニニ

12NW+00

END OF SLAB

STA. 12NW+21.75-

827+50

END OF SLAB

STA. 827+59.86

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
ACCEPTED William C. Duche SUR 02/15/17
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-37-48

STH 29 WB OVER PLOVER RIVER

BY

COUNTY MARATHON VILLAGE HATLEY

DESIGN SPEC.
REHABILITATION N/A

DESIGNED BRE DESIGN KRO BY AAS CKD. KRO

GENERAL
PLAN

1009-45-66

TRAFFIC DATA

ADT = 17,700 (2042) RDS = 70 MPH

ESTIMATE OF QUANTITIES

509.5100.S POLYMER OVERLAY 303 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HS-20 INVENTORY RATING: HS-29 OPERATING RATING: WISCONSIN STANDARD PERMIT

VEHICLE (Wis-SPV): 250 KIPS

40'-0" OVERLAY (POLYMER) LIMITS STAGE HI CONST. STAGE H2 CONST. 1'-6" 6'-0" 12'-0" 12'-0" 10'-0" 1'-6" SHOULDER LANE LANE SHOULDER - R STH 29 EB 1/4" MIN. -EXIST. SLOPED FACE OVERLAY-- LONGIT. POLYMER PARAPET "B", TYP. CROWN -CONST. JOINT OVERLAY-0.02% 0.02% EDGE OF

CROSS SECTION THRU ROADWAY

61'-6" BACK TO BACK OF ABUTMENTS

59'-0"

29'-6"

SPAN 2

828+00

· LONGIT.

CONST. JOINT

€ BRG.

E. ABUT.

- R STH 29 EB

-END OF SLAB

- PAVING

NOTCH, TYP.

STA. 828+17.77

29'-6"

SPAN 1

- CROWN LINE

W. ABUT.

-EXIST. SLOPED FACE PARAPET "B", TYP.

PLOVER RIVER

<u>PL AN</u> 2 SPAN FLAT SLAB

827+50

END OF SLAB

STA. 827+57.61

(LOOKING EAST)

CONSULTANT CONTACT

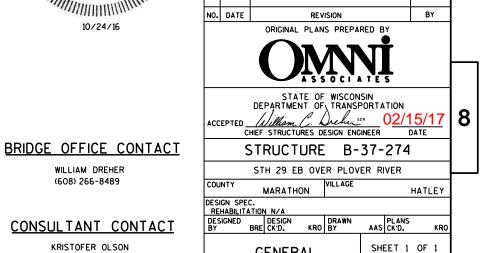
DECK (TYP.)

KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900

WILLIAM DREHER

(608) 266-8489

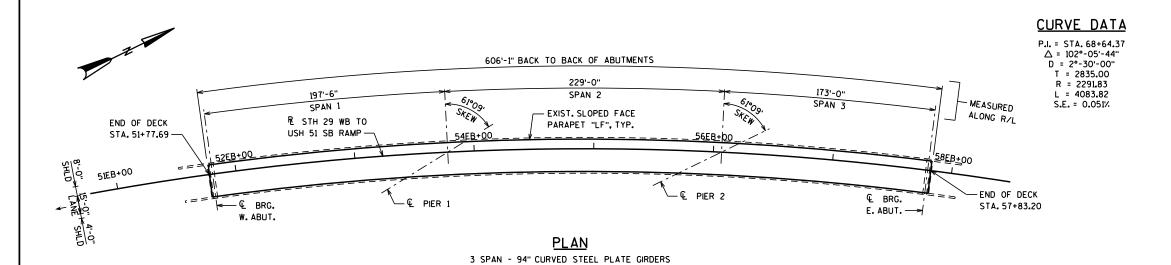
10/24/16



GENERAL

PLAN

1009-45-66



TRAFFIC DATA

ADT (2042) = 17.700 RDS = 70 MPH

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HS-25 INVENTORY RATING: HS-27 OPERATING RATING: HS-46 WISCONSIN STANDARD PERMIT VEHICLE (Wis-SPV): 250 KIPS

ESTIMATE OF QUANTITIES

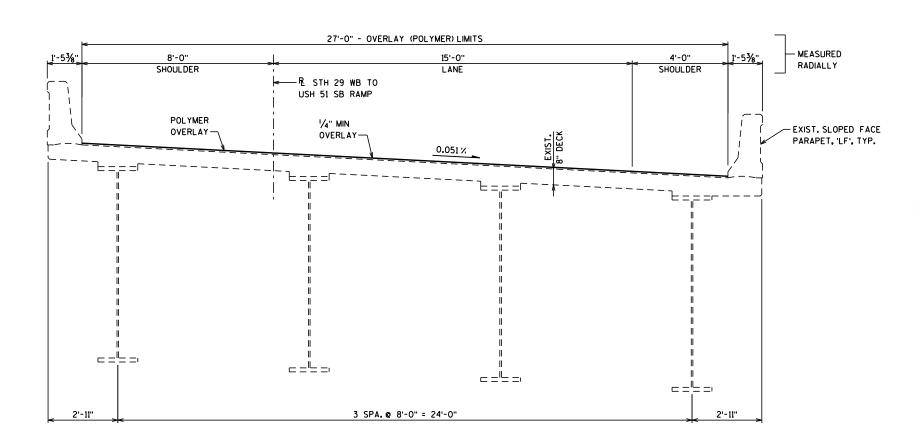
509.5100.S POLYMER OVERLAY 1859 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".



NO. DATE REVISION

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION 02/15/17

CHIEF STRUCTURES DESIGN ENGINEER STRUCTURE B-37-337

STH 29 WB TO USH 51 SB OVER USH 51

BY

COUNTY MARATHON RIB MOUNTAIN DESIGN SPEC. DESIGN SPEC.

REHABILITATION N/A

DESIGN BY BRE CK'D. KRO BY AAS CK'D. KRO SHEET 1 OF 1

GENERAL PLAN

BRIDGE OFFICE CONTACT

THE SCIONAL ET HILL

11/22/16

WILLIAM DREHER (608) 266-8489

CONSULTANT CONTACT

KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900

CROSS SECTION THRU ROADWAY

(LOOKING NORTHEAST)

1009-45-66

CURVE DATA

P.I. = STA 3115+65.54 △ = 88°-11'-26" D = 4°-0'-0" T = 1387.86 R = 1432.39 FT L = 2204.77 S.E. = 0.060 FT/FT P.C. = STA 3101+77.68 P.T. = STA 3123+82.44

TRAFFIC DATA

ADT = 29,800 (2042) RDS = 70 MPH

DESIGN DATA

LIVE LOAD:

MEASURED ALONG

TANGENT LINE

- END OF DECK

STA. 3107+01.32

- PAVING

NOTCH TYP.

3107+00

€ BRG. E. ABUT.

DESIGN LOADING: HS-25 INVENTORY RATING: HS-29 OPERATING RATING: HS-71 WISCONSIN STANDARD PERMIT VEHICLE (Wis-SPV): 250 KIPS

ESTIMATE OF QUANTITIES

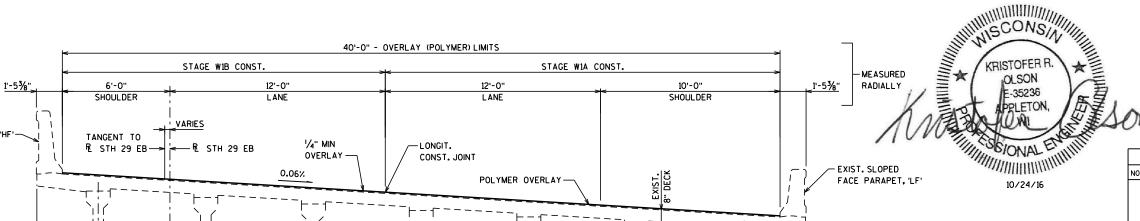
509.5100.S POLYMER OVERLAY 923 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".



200'-01/4" BACK TO BACK ABUTMENTS

196'-0"

3106+00

PLAN

2 SPAN - 45" PRESTRESSED GIRDER

- EXISTING

SLOPED FACE

PARAPET 'HF'

- EXISTING

SLOPED FACE

PARAPET 'LF'

SPAN 2

— № STH 29 EB

VARIES

12:56:44 PM

CROSS SECTION THRU ROADWAY (LOOKING EAST)

6 EQUAL SPACES

SPAN 1

- LONGIT. CONST. JOINT

-STRUCTURE & TANGENT TO

3105+00

EXIST. SLOPED

8

FACE PARAPET, 'HF'

END OF DECK

STA. 3105+02.50

- E BRG. W. ABUT.

ROADWAY R @ STA. 3106+00.84

CONSULTANT CONTACT

BRIDGE OFFICE CONTACT

WILLIAM DREHER

(608) 266-8489

KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900

NO. DATE REVISION BY STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED William C. Drehn SOR 02/15/17 8 STRUCTURE B-37-355

STH 29 EB OVER 28TH AVENUE

COUNTY MARATHON WAUSAU DESIGN SPEC. REHABILITATION N/A

DESIGNED DESIGN BY BRE CKD. KRO BY AAS CKD.

GENERAL

SHEET 1 OF 1 PLAN

VARIES



1009-45-66

CURVE DATA

P.I. = STA 4119+37.60 PC STA = 4102+55.35 PT STA = 4128+43.51 △ = 93° 10' 26" D = 3° 36' 0" T = 1682.25 L = 2588.16 R = 1591.55 S.E. = 0.059%

TRAFFIC DATA

ADT = 29,800 (2042) RDS = 70 MPH

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HS-25 INVENTORY RATING: HS-30 OPERATING RATING: HS-70 WISCONSIN STANDARD PERMIT VEHICLE (Wis-SPV): 250 KIPS

ESTIMATE OF QUANTITIES

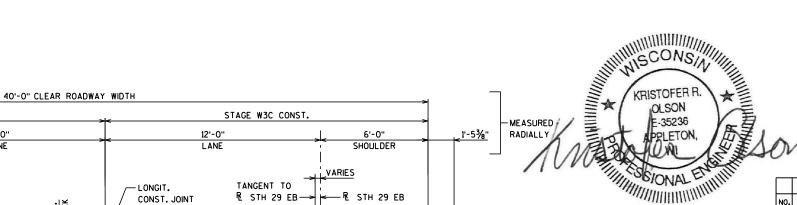
509.5100.S POLYMER OVERLAY 851 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".



90'-0"

- EXISTING SLOPED FACE PARAPET 'HF'

- EXISTING

SLOPED FACE

PARAPET 'LF'

SPAN 2

R STH 29 WB-

EXIST. SLOPED

FACE PARAPET, 'LF'

END OF DECK STA. 4106+86.60-

& BRG. E. ABUT.-

EXIST. SLOPED FACE PARAPET, 'HF' 1/4" MIN OVERLAY -POLYMER 0.059% OVERLAY -8 VARIES 5 EQUAL SPACES VARIES

LANE

90'-0"

SPAN 1

- LONGIT.

STAGE W3A CONST.

CONST. JOINT

BRIDGE OFFICE CONTACT

10/24/16

-MEASURED ALONG

-STRUCTURE & TANGENT

TO ROADWAY R @

STA. 4105+94.69

4107+00

TANGENT LINE

WILLIAM DREHER (608) 266-8489

CONSULTANT CONTACT

KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900



STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION ACCEPTED William C. Diches SDR 02/15/17 CHIEF STRUCTURES DESIGN ENGINEER

8

STRUCTURE B-37-356

STH 29 WB OVER 28TH AVENUE COUNTY MARATHON WAUSAU DESIGN SPEC. REHABILITATION N/A

DESIGNED BY BRE CK'D. KRO BY AAS CK'D. KRO

GENERAL

SHEET 1 OF 1 PLAN

CROSS SECTION THRU ROADWAY

(LOOKING EAST)

183'-11" BACK TO BACK ABUTMENTS

180'-0"

4106+00

PLAN

2 SPAN - 45" PRESTRESSED GIRDERS

-€ PIER

12°26'

PAVING

1'-5¾"

NOTCH TYP.

4105+00

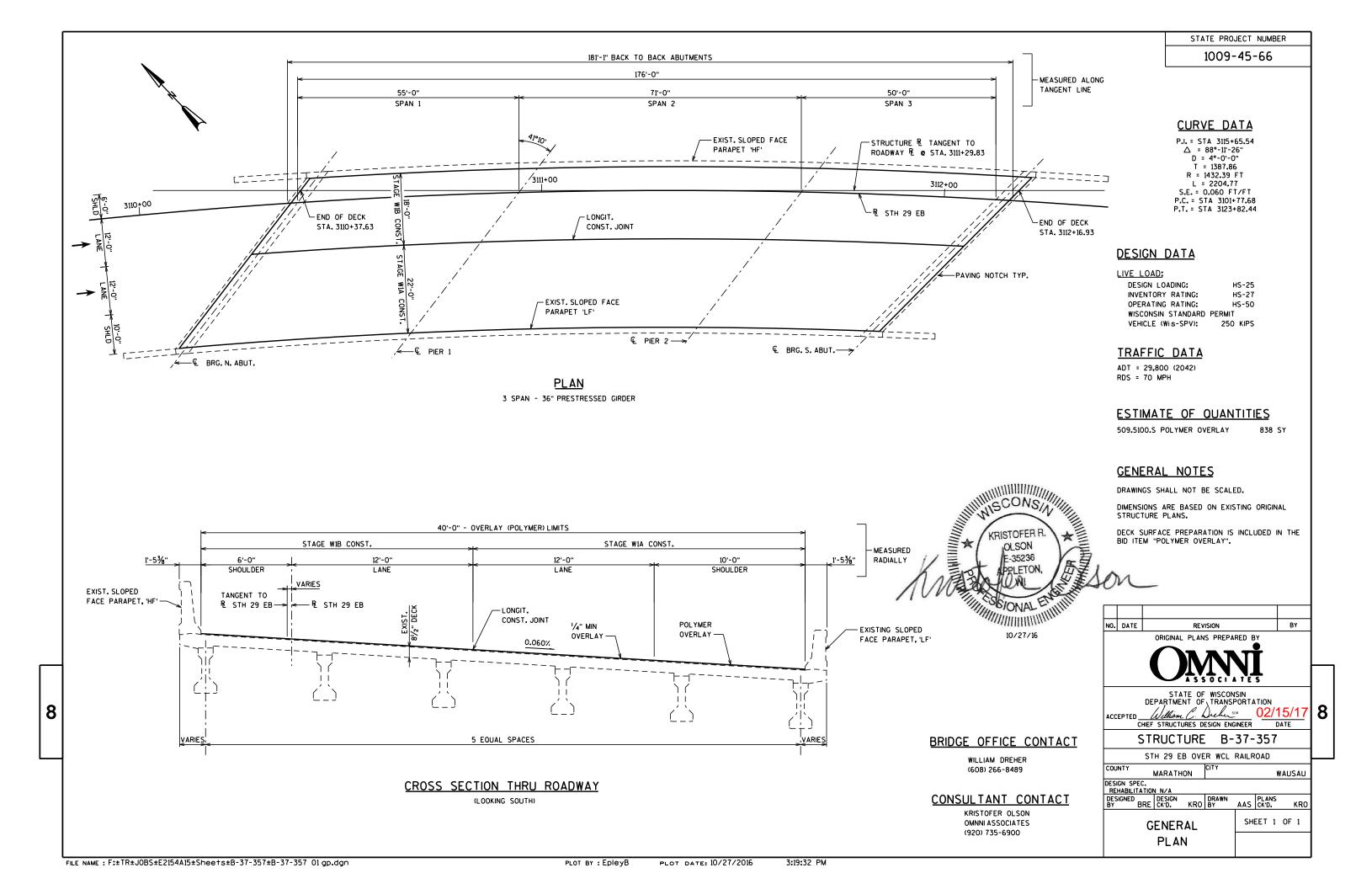
END OF DECK

STA. 4105+03.93

-Œ BRG.W.ABUT.

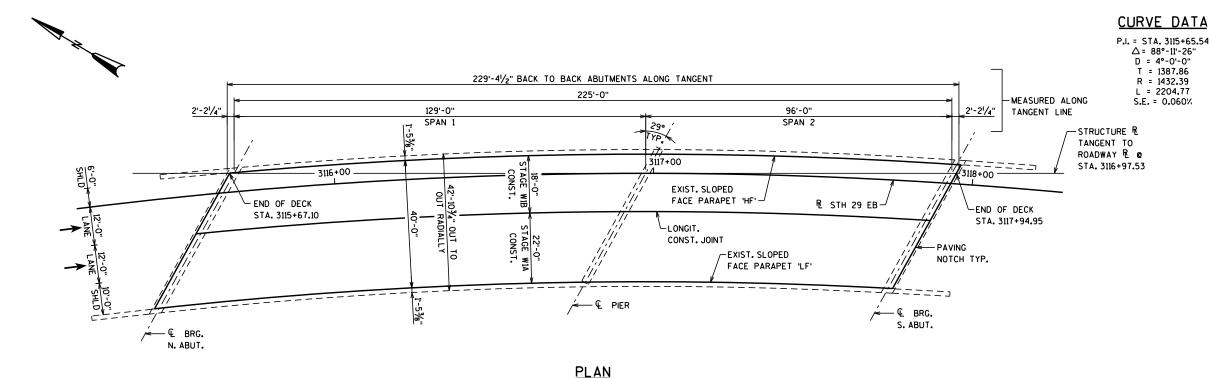
10'-0"

SHOULDER





1009-45-66



2 SPAN - 70" PRESTRESSED GIRDERS

DESIGN DATA

LIVE LOAD:

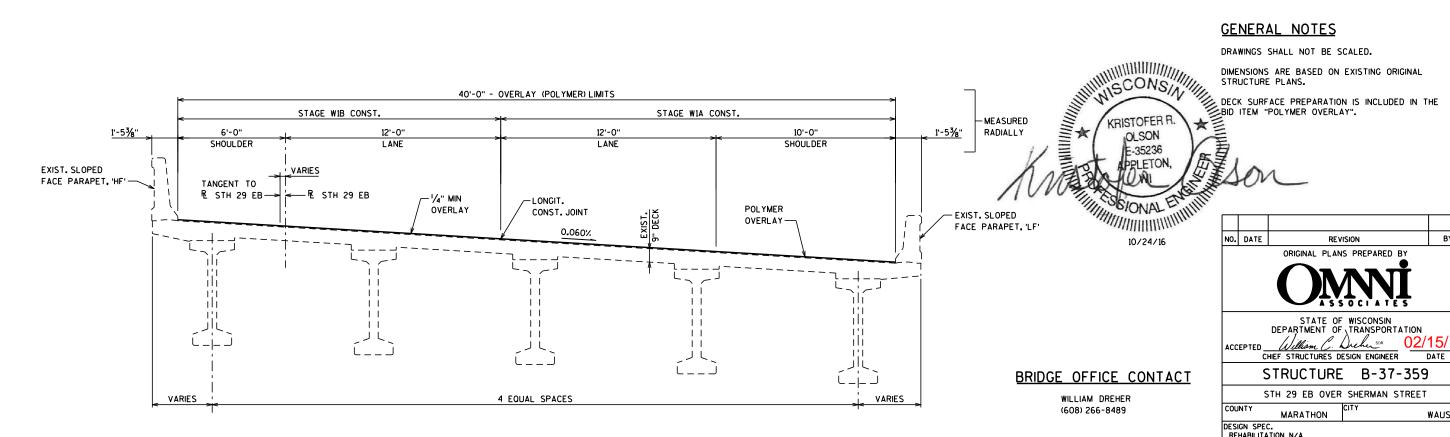
DESIGN LOADING: HS-20 INVENTORY RATING: HS-25 OPERATING RATING: HS-50 WISCONSIN STANDARD PERMIT VEHICLE (Wis-SPV): 225 KIPS

TRAFFIC DATA

ADT = 29,800 (2042) RDS = 70 MPH

ESTIMATE OF QUANTITIES

509.5100.S POLYMER OVERLAY 1055 SY



NO. DATE

10/24/16

CONSULTANT CONTACT

KRISTOFER OLSON

OMNNI ASSOCIATES

(920) 735-6900

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION D William C. Dieherson CHIEF STRUCTURES DESIGN ENGINEER 02/15/17 ACCEPTED_

REVISION

BY

8

STRUCTURE B-37-359

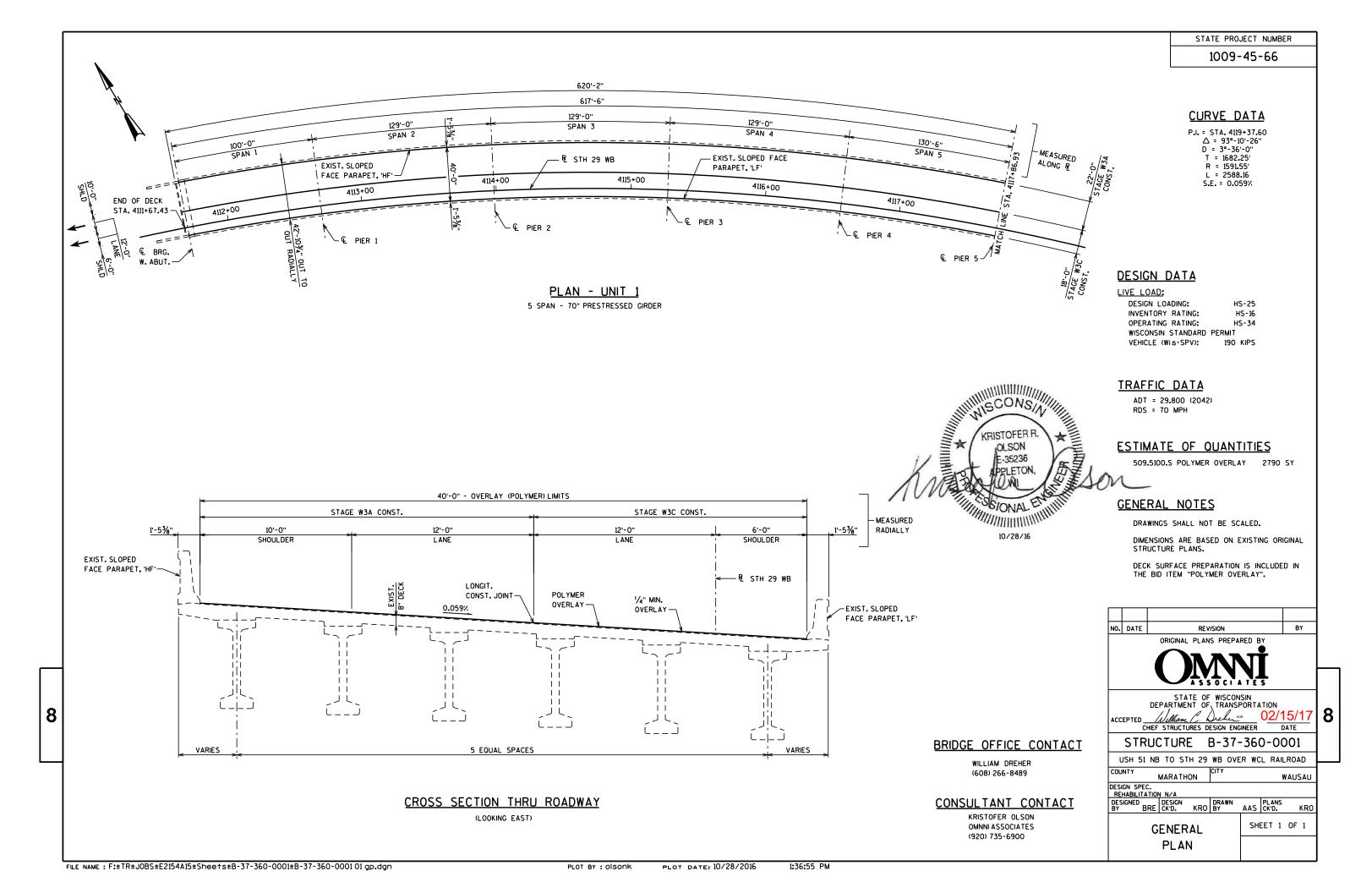
STH 29 EB OVER SHERMAN STREET

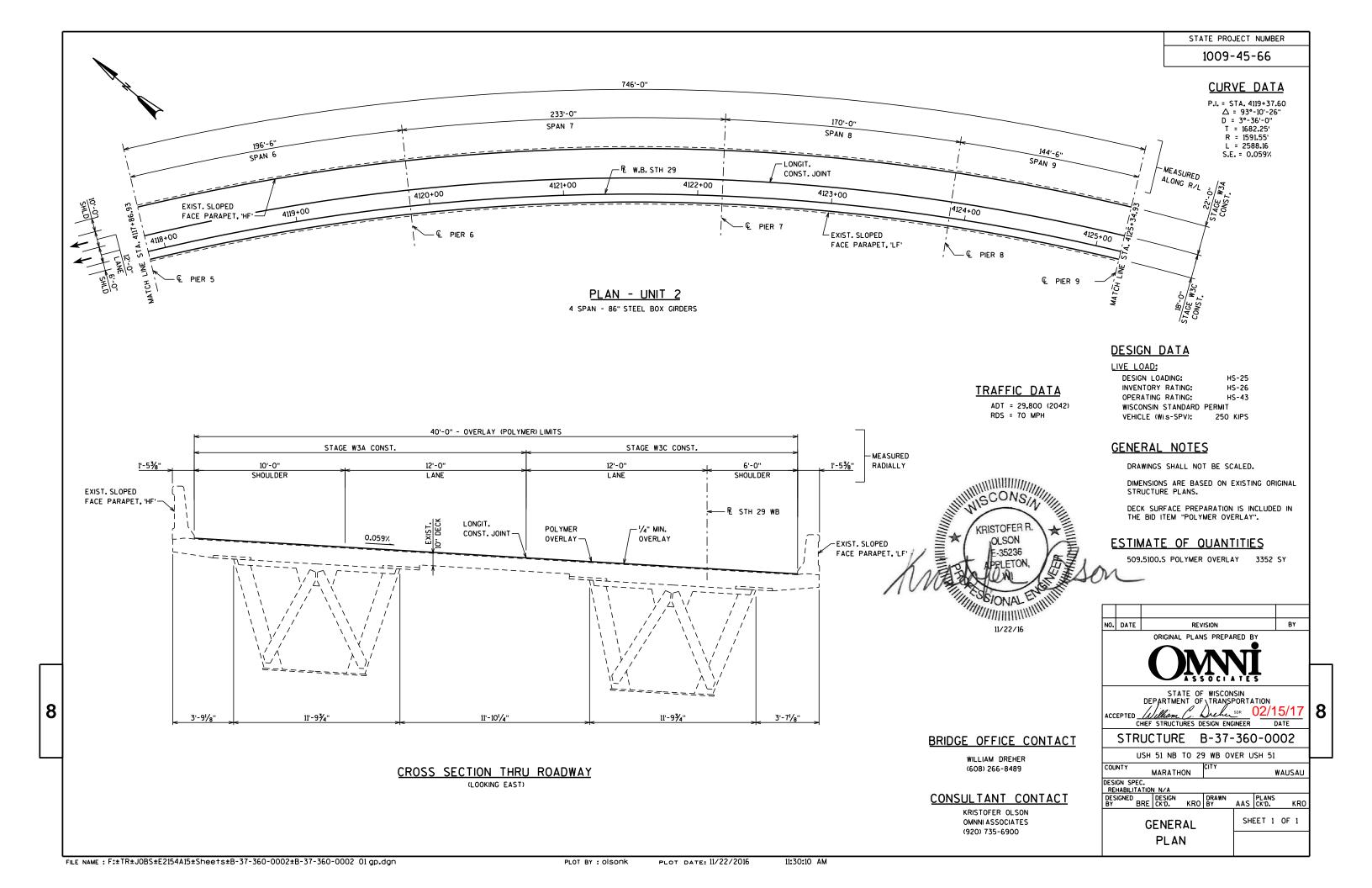
COUNTY MARATHON WAUSAU DESIGN SPEC. REHABILITATION N/A

DESIGNED DESIGN BY BRE CK'D. KRO BY AAS CK'D. SHEET 1 OF 1 **GENERAL** PLAN

CROSS SECTION THRU ROADWAY

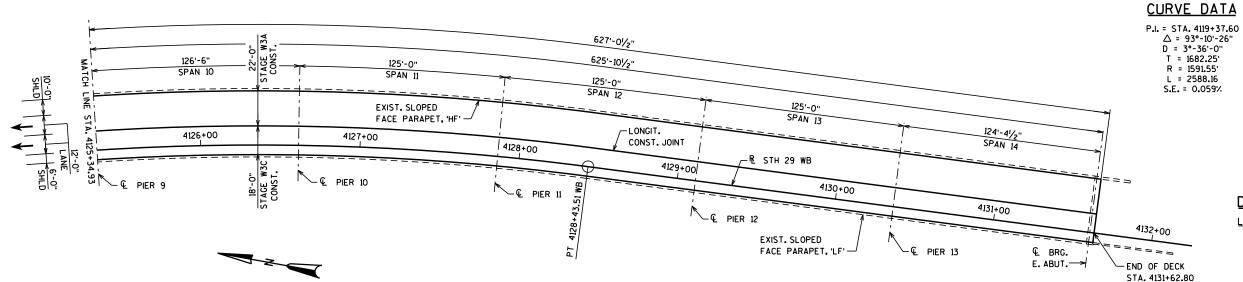
(LOOKING SOUTH)







1009-45-66



PLAN - UNIT 3

5 SPAN - 70" PRESTRESSED GIRDERS

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HS-25 INVENTORY RATING: HS-17 OPERATING RATING: HS-36 WISCONSIN STANDARD PERMIT VEHICLE (Wis-SPV): 190 KIPS

TRAFFIC DATA

ADT = 29,800 (2042) RDS = 70 MPH

ESTIMATE OF QUANTITIES

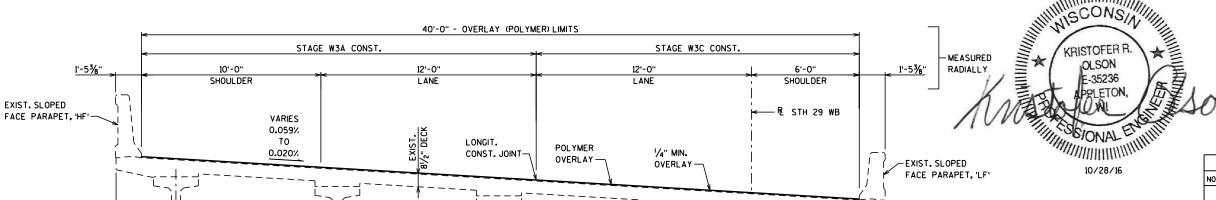
509.5100.S POLYMER OVERLAY 2827 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".



BRIDGE OFFICE CONTACT

WILLIAM DREHER (608) 266-8489

CONSULTANT CONTACT

OMNNI ASSOCIATES (920) 735-6900

NO. DATE REVISION BY STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

C. Diches 02/15/17 D William C. Dreheiser
CHIEF STRUCTURES DESIGN ENGINEER

8

STRUCTURE B-37-360-0003

USH 51 NB TO STH 29 WB OVER LAND

COUNTY MARATHON WAUSAU DESIGN SPEC. REHABILITATION N/A

DESIGNED BY BRE CK'D. KRO BY AAS CK'D. KRO SHEET 1 OF 1 **GENERAL**

PLAN

KRISTOFER OLSON

VARIES

8

4 EQUAL SPACES

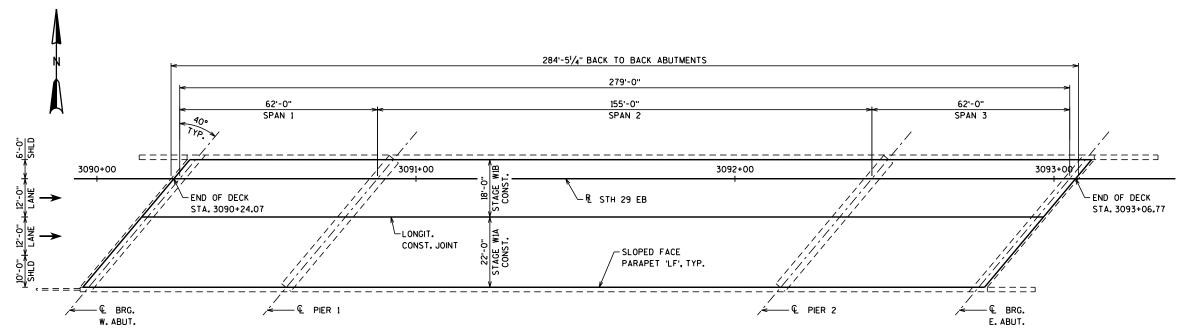
CROSS SECTION THRU ROADWAY

(LOOKING SOUTH)

VARIES

STATE PROJECT NUMBER

1009-45-66



PLAN

3 SPAN 72W" PRESTRESSED GIRDERS

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HS-20 INVENTORY RATING: HS-20 OPERATING RATING: HS-44 WISCONSIN STANDARD PERMIT VEHICLE (Wis-SPV): 240 KIPS

TRAFFIC DATA

ADT = 29,800 (2042) RDS = 70 MPH

ESTIMATE OF QUANTITIES

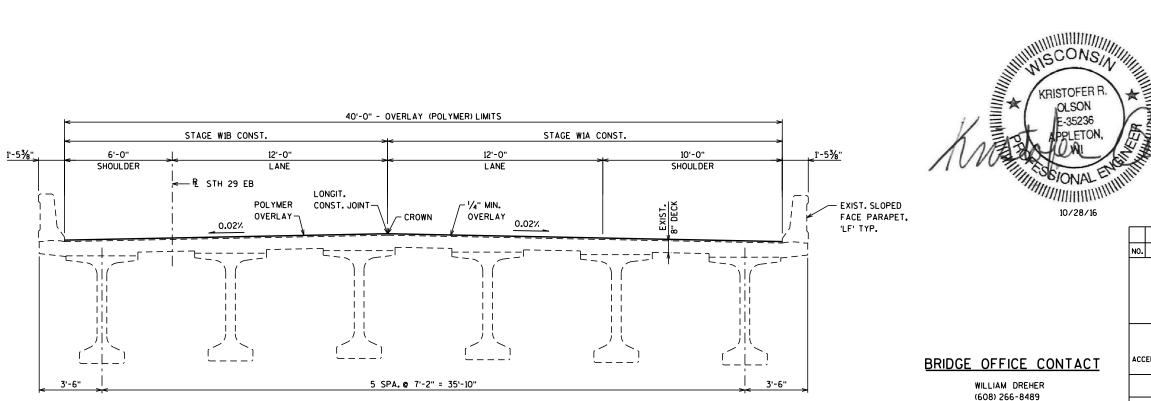
509.5100.S POLYMER OVERLAY 1302 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".



CROSS SECTION THRU ROADWAY (LOOKING EAST)

BRIDGE OFFICE CONTACT

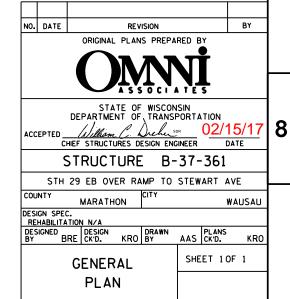
MINIMUM BONAL EMILIE

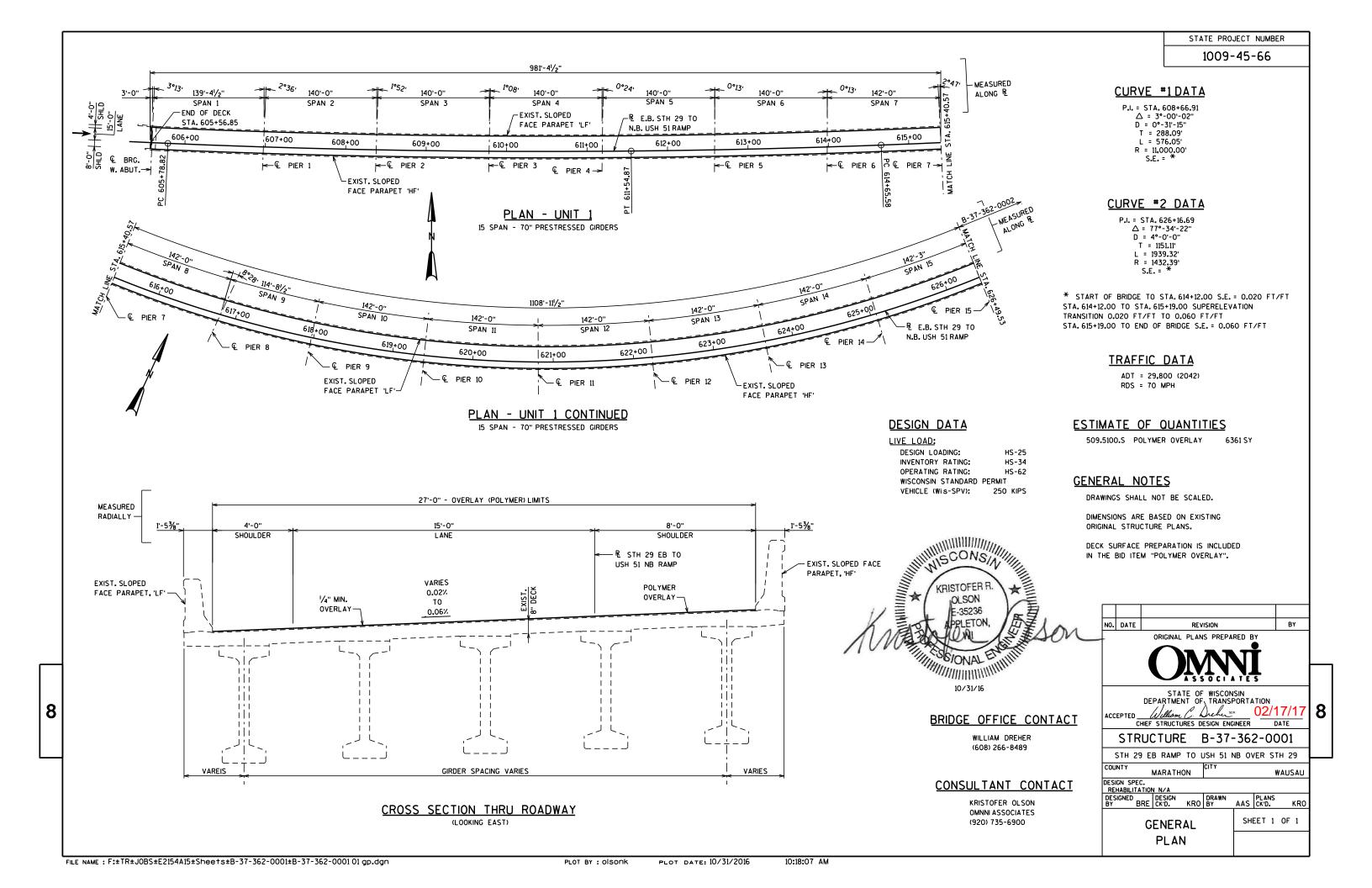
10/28/16

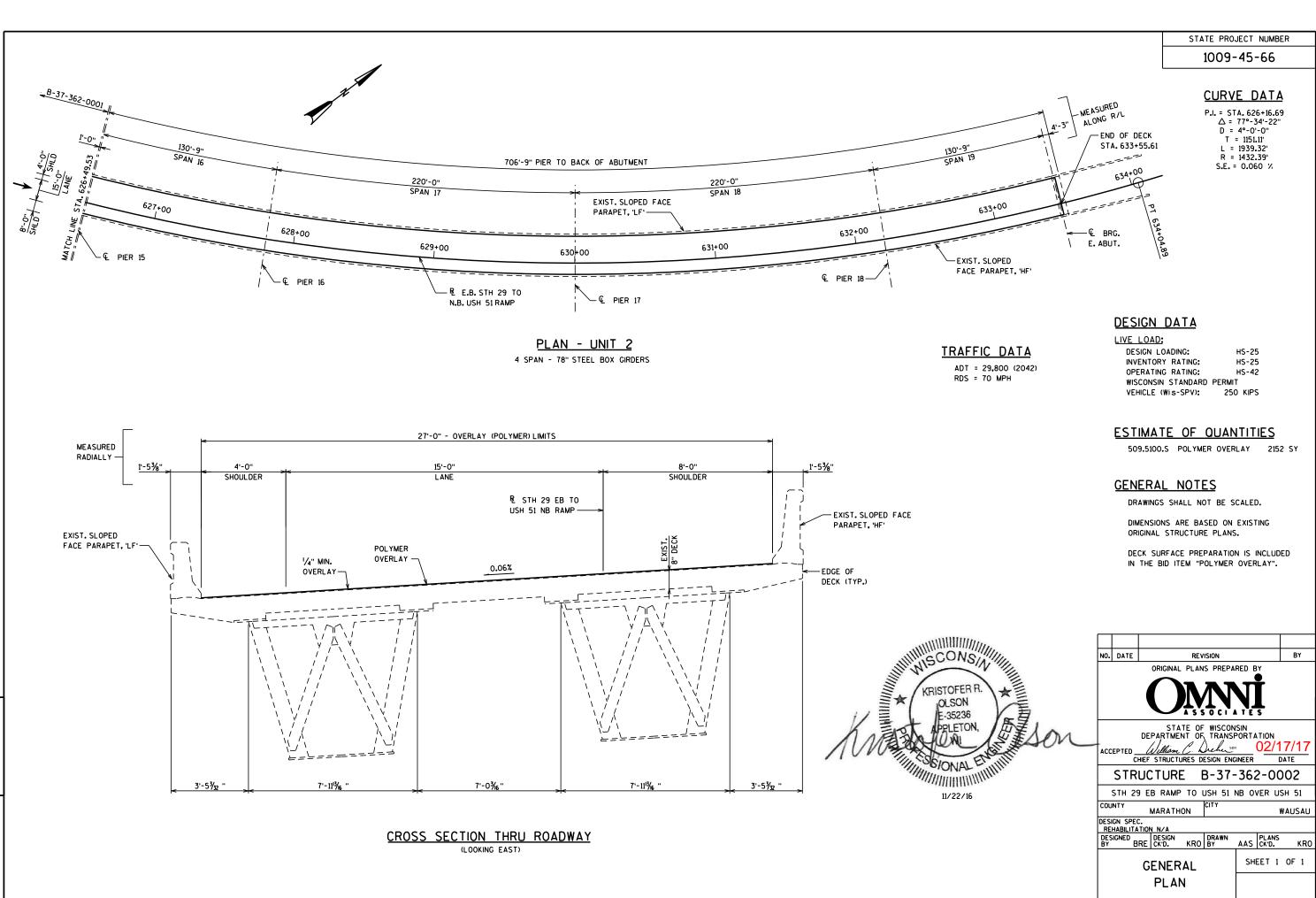
WILLIAM DREHER (608) 266-8489

CONSULTANT CONTACT

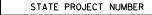
KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900

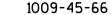


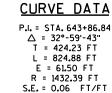




8







MEASURED ALONG

TANGENT LINE

NOTCH, TYP.

END OF DECK

STA. 648+13.67

€ BRG.

E. ABUT.

- PAVING

89'-41/8"

OF DECK

EDGE

OF DECK

648+00

TRAFFIC DATA

ADT = 29,800 (2042) RDS = 70 MPH

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HS-25
INVENTORY RATING: HS-21
OPERATING RATING: HS-44
WISCONSIN STANDARD PERMIT
VEHICLE (Wis-SPV): 240 KIPS

ESTIMATE OF QUANTITIES

509 . 5100 . S	POLYMER OVERLAY — 902	SY
509.9025.S	EPOXY INJECTION CRACK REPAIR 100	LF
509.9026.S	CORED HOLES 2-INCH DIAMETER — 4	EΑ
517.1010.S	CONCRETE STAINING B-37-364 1,700	SF

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

FOLLOWING EPOXY CRACK INJECTION AND CORING, STAIN ALL PIER SURFACES THAT ARE CURRENTLY STAINED. STAINING OF NON-PIER CONCRETE SURFACES IS NOT REQUIRED.

RRISTOFER R.
OLSON
E-35236
PPLETON,
PPLETON,
PPLETON,
PRAPET 'LF'
10/31/16

BRIDGE OFFICE CONTACT

WILLIAM DREHER (608) 266-8489

CONSULTANT CONTACT

KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900



STRUCTURE B-37-364

RAMP BRIDGE FROM USH 51 SB TO WB STH 29

DESIGN SPEC.
REHABILITATION N/A

DESIGNED BRE CK'D. KRO BY AAS CK'D. KRO

GENERAL
PLAN

PLAN

SHEET 1 OF 1

CROSS SECTION THRU ROADWAY

(LOOKING NORTHEAST)

4 SPA. @ 6'-1" = 24'-4"

FILE NAME: F:+TR+JOBS+E2154A15+Shee+s+B-37-364+B-37-364 01 gp.dgn

VARIES

STRUCTURE & TANGENT TO

645+00

END OF DECK

MSE RETAINING

WALL, TYP.

STA, 645+23,59

ROADWAY R @ 646+68.63 -

MEASURED

RADIALLY -

EXIST. SLOPED

8

FACE PARAPET 'HF'

PLOT BY : EpleyB

290'-10¾" BACK TO BACK ABUTMENTS

287'-03/4"

108'-3"

SPAN 2

PLAN

3 SPAN - 45" PRESTRESSED GIRDERS

647±00

PIER REPAIR

← C PIER 2

FACE PARAPET 'LF'

4'-0"

SHOULDER

- 1/4" MIN.

OVERLAY

j'-5<u>%</u>''

VARIES

EXIST. SLOPED

P.O.T. STA. 646+68.63

-EXIST. SLOPED

- C PIER 1

27'-0" - OVERLAY (POLYMER) LIMITS

LANE

0.06%

POLYMER

OVERLAY-

FACE PARAPET 'HE'

89'-4%"

SPAN 1

646F+00 STH 52 TO USH 51
SOUTHBOUND TRAFFIC

- & BRG. W. ABUT

SHOULDER

TANGENT TO

R STH 29 EB-

PIER REPAIR -

VARIES

- R STH 29 EB

GIRDERS, TYP.

646+00 | |

PLOT DATE: 10/31/2016

2:02:43 PM

STATE PROJECT NUMBER

1009-45-66

TRAFFIC DATA

ADT = 29,800 (2042) RDS = 70 MPH

ESTIMATE OF QUANTITIES

509.5100.S POLYMER OVERLAY 1065 SY

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: INVENTORY RATING:

OPERATING RATING: WISCONSIN STANDARD PERMIT

VEHICLE (Wis-SPV): 250 KIPS

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".

BRIDGE OFFICE CONTACT

HS-20

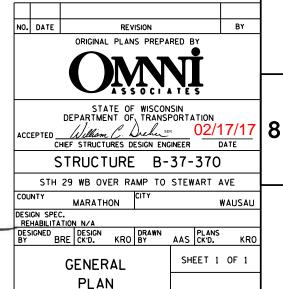
HS-21

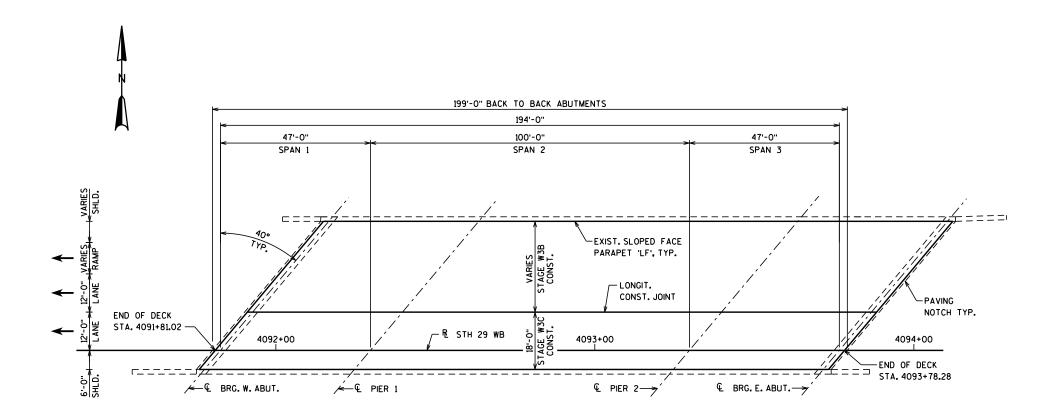
HS-46

WILLIAM DREHER (608) 266-8489

CONSULTANT CONTACT

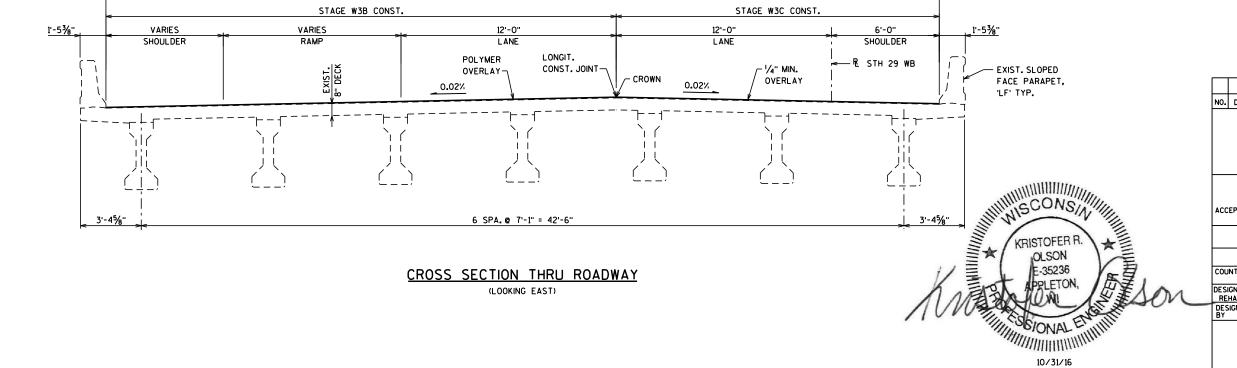
KRISTOFER OLSON OMNNI ASSOCIATES (920) 735-6900





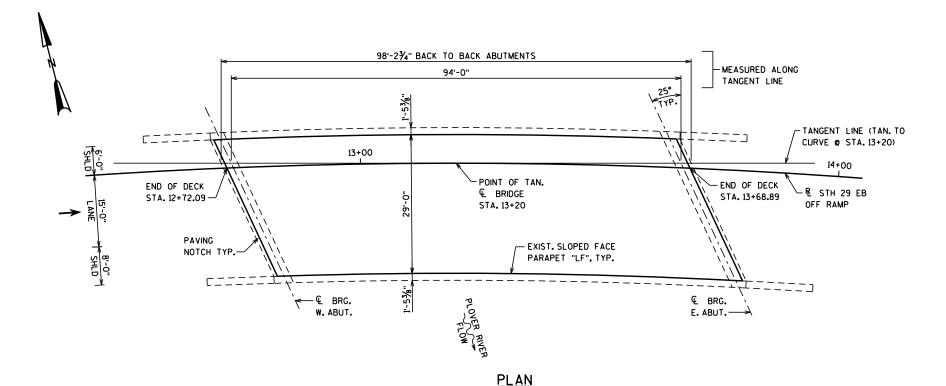
PLAN
3 SPAN 45" PRESTRESSED GIRDERS

46'-51/4" - OVERLAY (POLYMER) LIMITS



STATE PROJECT NUMBER

1009-45-66



SINGLE SPAN 45" PRESTRESSED GIRDER

29'-0" - OVERLAY (POLYMER) LIMITS

CURVE DATA

P.I. = STA. 11+40.57

△= 26°-38'-36.8" D = 4°-59'-58.7"

T = 271.36' L = 532.91' R = 1.146.00' S.E. = 0.04 %

P.C. = STA 8+69.20

P.T. = STA 14+02.11

ADT = 2,065 (2042) RDS = 70 M.P.H.

TRAFFIC DATA

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HS-20 INVENTORY RATING: HS-36 OPERATING RATING: HS-52 WISCONSIN STANDARD PERMIT VEHICLE (Wis-SPV): 250 KIPS

ESTIMATE OF QUANTITIES

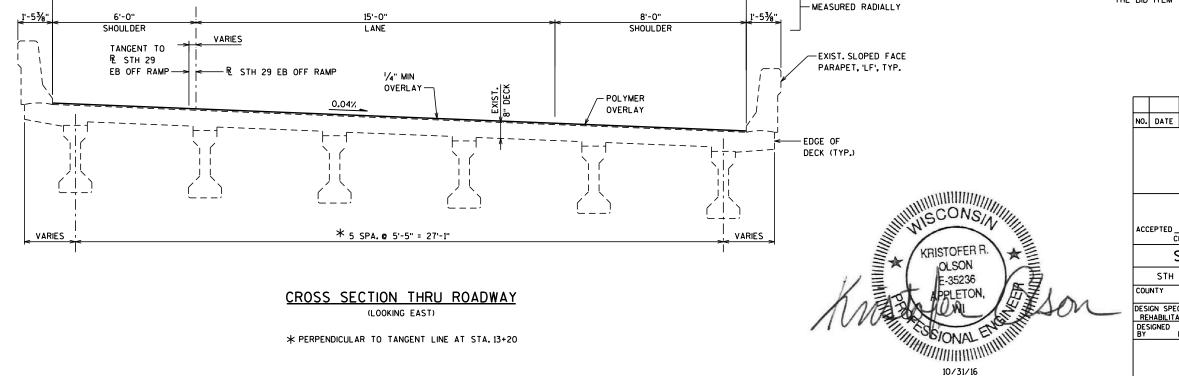
509.5100.S POLYMER OVERLAY 340 SY

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS ARE BASED ON EXISTING ORIGINAL STRUCTURE PLANS.

DECK SURFACE PREPARATION IS INCLUDED IN THE BID ITEM "POLYMER OVERLAY".



STH 29 EB OFF RAMP OVER PLOVER RIVER

MARATHON NORRIE DESIGN SPEC. REHABILITATION N/A DESIGNED BRE CK'D. PLANS CK'D. SHEET 1 OF 1 **GENERAL** PLAN

Notes



Wisconsin Department of Transportation

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