

NWL

NOVEMBER 2017

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile (Includes Erosion Control Plan)
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 62



DESIGN DESIGNATION

A.A.D.T. 2018	=	340
A.A.D.T. 2038	=	370
D.H.V.	=	40
D.D.	=	60-40
T.	=	5%
DESIGN SPEED	=	45 MPH
ESALS	=	61,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	PL + 58.1
PROPERTY LINE	PL + 58.1
LOT LINE	PL + 58.1
LIMITED HIGHWAY EASEMENT	PL + 58.1
EXISTING RIGHT OF WAY	PL + 58.1
PROPOSED OR NEW R/W LINE	PL + 58.1
SLOPE INTERCEPT	PL + 58.1
REFERENCE LINE	PL + 58.1
EXISTING CULVERT	PL + 58.1
PROPOSED CULVERT (Box or Pipe)	PL + 58.1
COMBUSTIBLE FLUIDS	CAUTION
MARSH AREA	PL + 58.1
WOODED OR SHRUB AREA	PL + 58.1
HIGH VOLTAGE	PL + 58.1

PROFILE	
GRADE LINE	PL + 58.1
ORIGINAL GROUND	PL + 58.1
MARSH OR ROCK PROFILE (To be noted as such)	PL + 58.1
SPECIAL DITCH	PL + 58.1
GRADE ELEVATION	PL + 58.1
CULVERT (Profile View)	PL + 58.1
UTILITIES	PL + 58.1
OVERHEAD UTILITIES	PL + 58.1
ELECTRIC	PL + 58.1
FIBER OPTIC	PL + 58.1
GAS	PL + 58.1
SANITARY SEWER	PL + 58.1
STORM SEWER	PL + 58.1
TELEPHONE	PL + 58.1
WATER	PL + 58.1
UTILITY PEDESTAL	PL + 58.1
POWER POLE	PL + 58.1
TELEPHONE POLE	PL + 58.1

ROCK	PL + 58.1
LABEL	PL + 58.1
OH	PL + 58.1
E	PL + 58.1
FO	PL + 58.1
G	PL + 58.1
SAN	PL + 58.1
SS	PL + 58.1
T	PL + 58.1
W	PL + 58.1
PL + 58.1	PL + 58.1
PL + 58.1	PL + 58.1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

CTH I - E COUNTY LINE

LITTLE JUMP RIVER BRIDGE B-54-0125

CTH V
RUSK COUNTY

STRUCTURE B-54-125

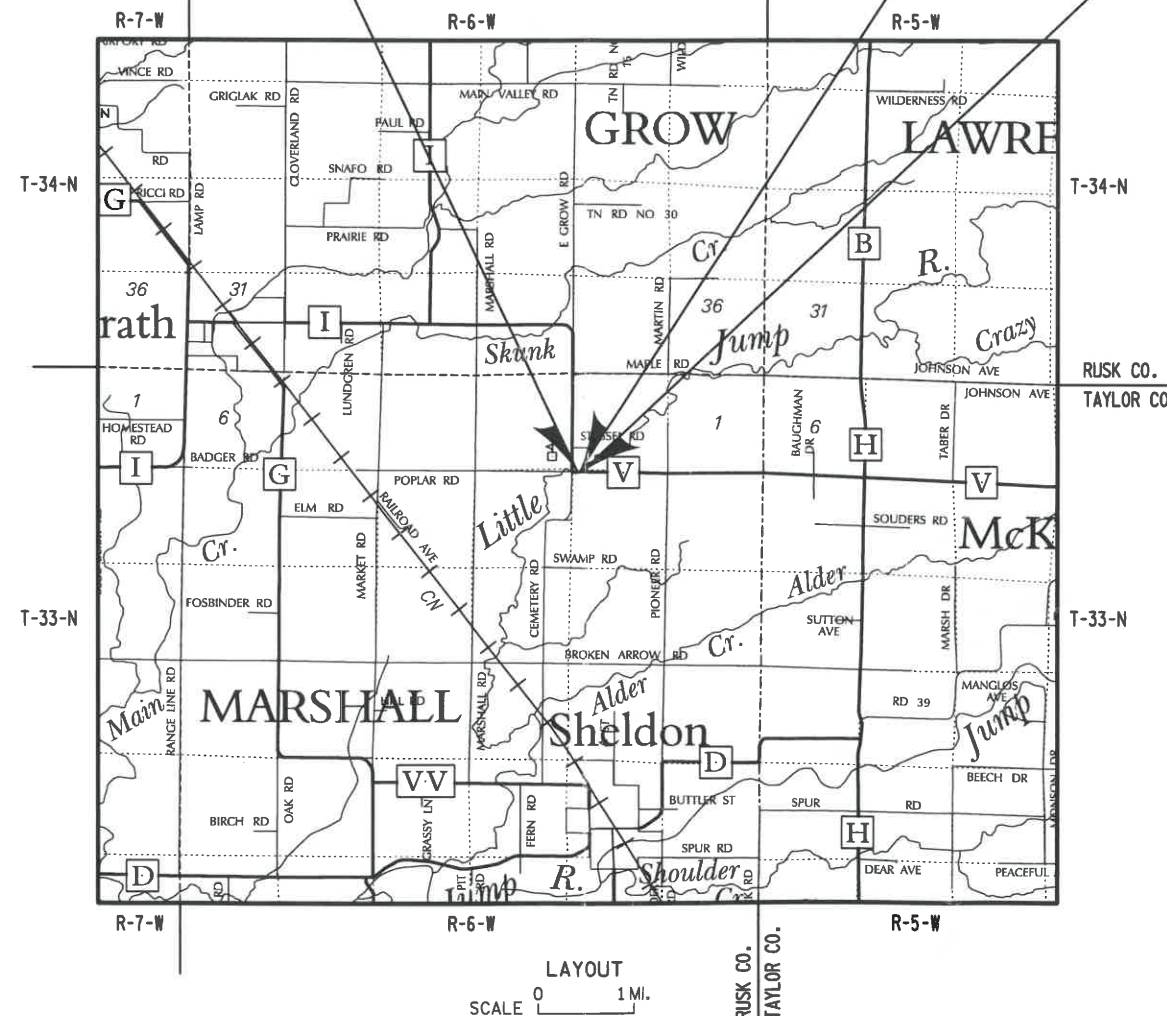
BEGIN PROJECT 8796-00-70

STA. 8+10.00
Y=527058.757
X=848045.582

STATE PROJECT NUMBER
8796-00-70

END PROJECT 8796-00-70

STA. 11+95.00



TOTAL NET LENGTH OF CENTERLINE = 0.073 MI.

COORDINATES ON THIS PLAN ARE REFERENCED TO THE WISCONSIN
COUNTY COORDINATE SYSTEM (WCCS), RUSK COUNTY, NAD 83 (2011).
ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO NAVD 88.

STATE PROJECT

8796-00-70

FEDERAL PROJECT

PROJECT

WISC 2017526

CONTRACT

1

ACCEPTED FOR
COUNTY OF
RUSK
BY

DATE: 4/6/17
HIGHWAY COMMISSIONER

ORIGINAL PLANS PREPARED BY

AECOM



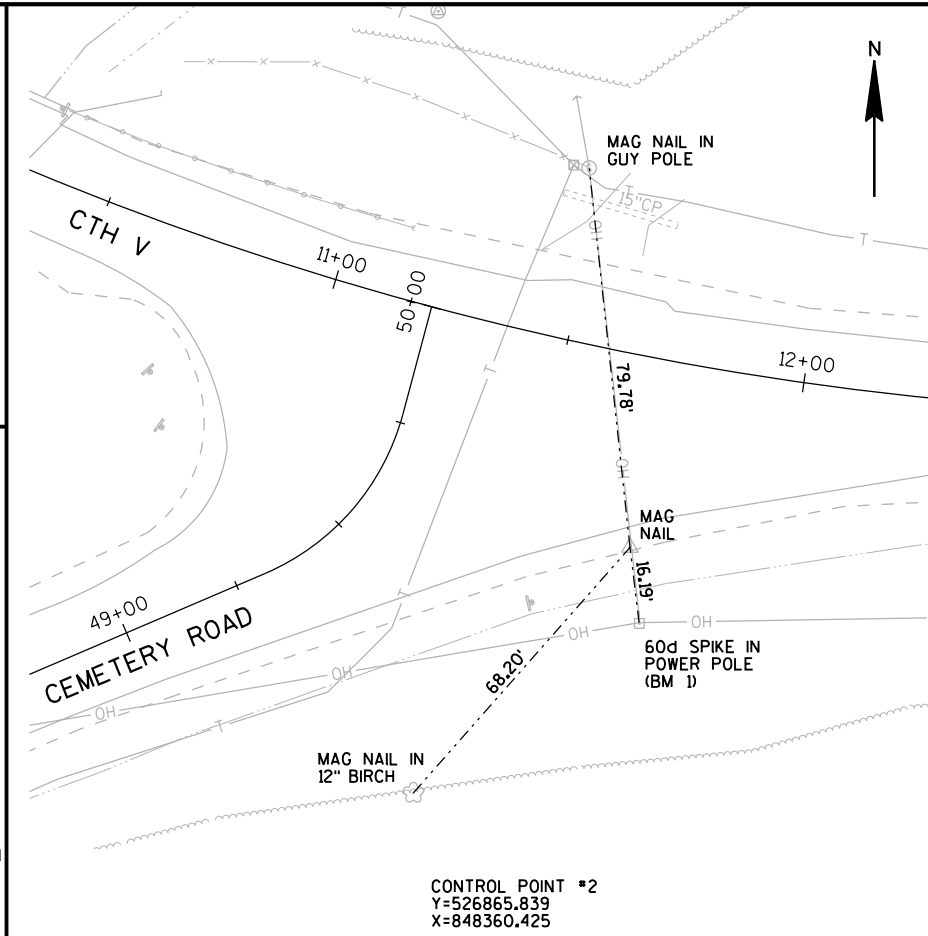
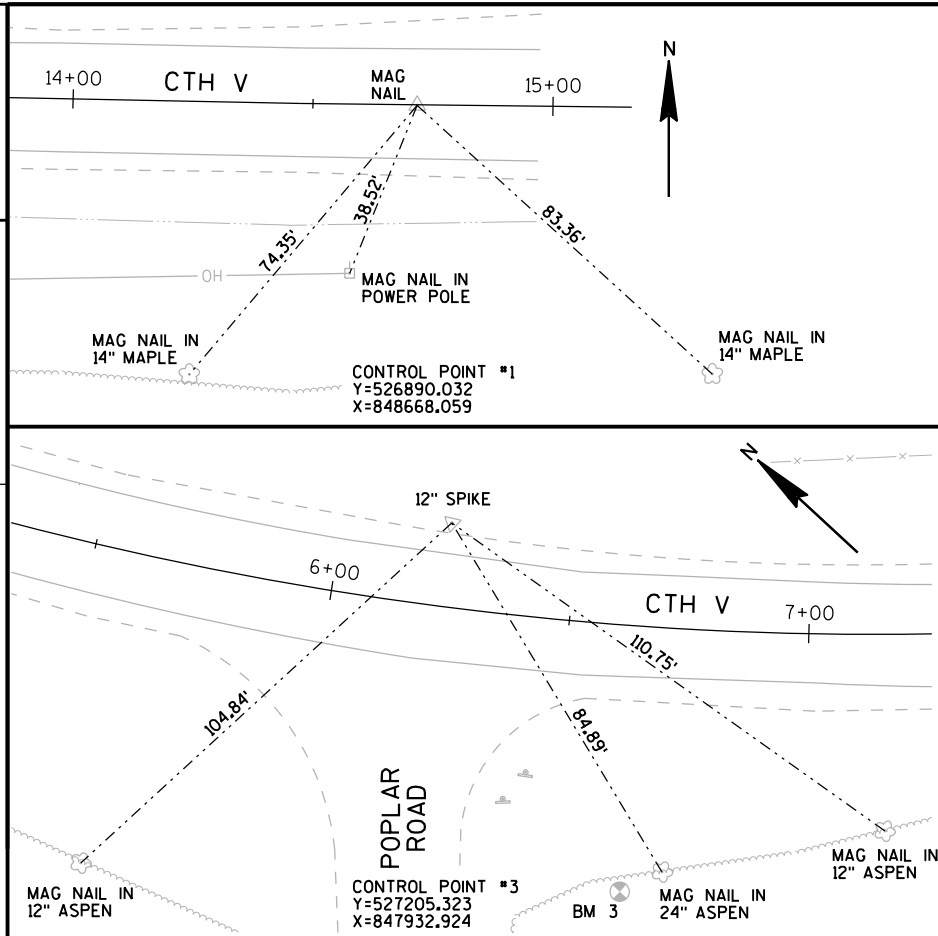
DATE: 4/6/17
(Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor: AECOM
Designer: AECOM
Management Consultant: KNIGHT E/A INC.

APPROVED FOR THE DEPARTMENT
DATE: 4/20/17
(Management Consultant Signature)

E



GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

PROVIDE SEED MIXTURE AS SHOWN ON THE TYPICAL SECTIONS.

WHERE THE QUANTITY OF BASE AGGREGATE DENSE AND ASPHALTIC SURFACE IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE, AS SHOWN ON THE PLANS, IS APPROXIMATE. THE ACTUAL THICKNESS WILL DEPEND UPON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE AND IS NOT SHOWN ON THE CROSS SECTIONS BUT IF REQUIRED, SHALL BE MEASURED AND PAID FOR AS EXCAVATION COMMON. LOCATION FOR EBS WILL BE DETERMINED BY THE ENGINEER.

SECTIONS AS SHOWN ON THE CROSS SECTION SHEETS INCLUDE THE THICKNESS OF TOPSOIL.

DISTURBED AREAS WITHIN THE RIGHT OF WAY, EXCEPT THE AREA WITHIN THE FINISHED SHOULDER POINTS, SHALL BE FERTILIZED, SEEDED AS DIRECTED BY THE ENGINEER.

EROSION CONTROL FEATURES AS SHOWN ON THE PLANS ARE SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

FIELD ADJUST SILT FENCE TO FIT EXISTING CONDITIONS AS SHOWN ON THE PLANS.

DISTANCES SHOWN ON THIS PLAN ARE GROUND DISTANCES.

FILL AS SHOWN ON THE PLAN SHEETS PERTAINS TO EMBANKMENTS CONSTRUCTED FROM EXCAVATION COMMON. THE SHRINKAGE ALLOWANCE USED TO COMPUTE THE VOLUME OF MATERIAL NECESSARY TO COMPLETE THE FILL IS 25 PERCENT.

THE RUNOFF COEFFICIENTS OF SURFACE DRAINAGE AT THE PROJECT SITE WILL NOT BE CHANGED FROM BEFORE TO AFTER CONSTRUCTION. THE TOTAL AREA IS 0.806 ACRE AND THE TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES IS 0.666 ACRE.

WETLANDS ARE PRESENT WITHIN THE PROJECT LIMITS. DO NOT OPERATE EQUIPMENT OUTSIDE THE SLOPE INTERCEPTS.

THE WISCONSIN DEPARTMENT OF TRANSPORTATION WILL FURNISH THE CONTRACTOR AN ALUMINUM MONUMENT TO SET IN THE STRUCTURE AS DESIGNATED BY THE ENGINEER.

THE ASPHALTIC SURFACE WEIGHT CALCULATIONS ARE BASED ON A UNIT WEIGHT OF 112 LBS/SY/IN OF DEPTH.

THE BASE AGGREGATE DENSE (BAD) 3/4-INCH WEIGHT CALCUATIONS ARE BASED ON A UNIT WEIGHT OF 2.1 TONS/CY.

THE BASE AGGREGATE DENSE (BAD) 1 1/4-INCH WEIGHT CALCUATIONS ARE BASED ON A UNIT WEIGHT OF 2.0 TONS/CY.

STANDARD ABBREVIATIONS

AVG	AVERAGE	EXC	EXCAVATION	REQ'D	REQUIRED
BK	BACK	EXIST	EXISTING	RL OR R	REFERENCE LINE
BM	BENCH MARK	FE	FIELD ENTRANCE	RT	RIGHT
CL OR C	CENTER LINE	L	LENGTH OF CURVE	R/W	RIGHT OF WAY
△	CENTRAL ANGLE OR DELTA	N	NORTH	RD	ROAD
CE	COMMERCIAL ENTRANCE	PC	POINT OF CURVATURE	S	SOUTH
CTH	COUNTY TRUNK HIGHWAY	PI	POINT OF INTERSECTION	STA	STATION
CY OR CUYD	CUBIC YARD	PT	POINT OF TANGENCY	TLE	TEMPORARY LIMITED EASEMENT
CP	CULVERT PIPE	PE	PRIVATE ENTRANCE	T	TRUCKS (PERCENT OF)
D	DEGREE OF CURVE	PL	PROPERTY LINE	TYP	TYPICAL
DHV	DESIGN HOUR VOLUME	PVC	VERTICAL POINT OF CURVE	USH	UNITED STATES HIGHWAY
DD	DIRECTIONAL DISTRIBUTION	PVI	VERTICAL POINT OF INTERSECTION	VC	VERTICAL CURVE
E	EAST	PVT	VERTICAL POINT OF TANGENCY	W	WEST
EL OR ELEV	ELEVATION	R	RADIUS		

UTILITY CONTACTS

CENTURYLINK (TELEPHONE)
ATTN: BRIAN HUHN
425 ELLINGSON AVENUE
P.O. BOX 78
HAWKINS, WI 54530
PHONE: (715) 532-0023
brian.huhn@centurylink.com

JUMP RIVER ELECTRIC COOP (ELECTRIC)
ATTN: DAVE OELKERS
1102 W. 9TH STREET NORTH
P.O. BOX 99
LADYSMITH, WI 54848
PHONE: 715-532-5524
jrec@jrec.net

COUNTY CONTACT

RUSK COUNTY HIGHWAY DEPARTMENT
ATTN: SCOTT EMCH
RUSK COUNTY HIGHWAY COMMISSIONER
N4711 ST 27
LADYSMITH, WI 54848
(715) 532-2633
semch@ruskcountywi.us

WDNR CONTACT

DEPARTMENT OF NATURAL RESOURCES
ATTN: AMY CRONK
810 W. MAPLE STREET
SPOONER, WI 54801
(715)-635-4229
amy.cronk@wisconsin.gov

ORDER OF SECTION 2 DETAIL SHEETS
GENERAL NOTES
TYPICAL SECTIONS
CONSTRUCTION DETAILS
PLAN DETAIL
ALIGNMENT DETAIL

DESIGNER CONTACT

AECOM
ATTN: BETH NEMEC
200 INDIANA AVENUE
STEVENS POINT, WI 54481
(715)-342-3069
elizabeth.nemec@aecom.com
AECOM PROJECT NO. 60428202

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 0.806 ACRES

TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.666 ACRES

PROJECT NUMBER: 8796-00-70

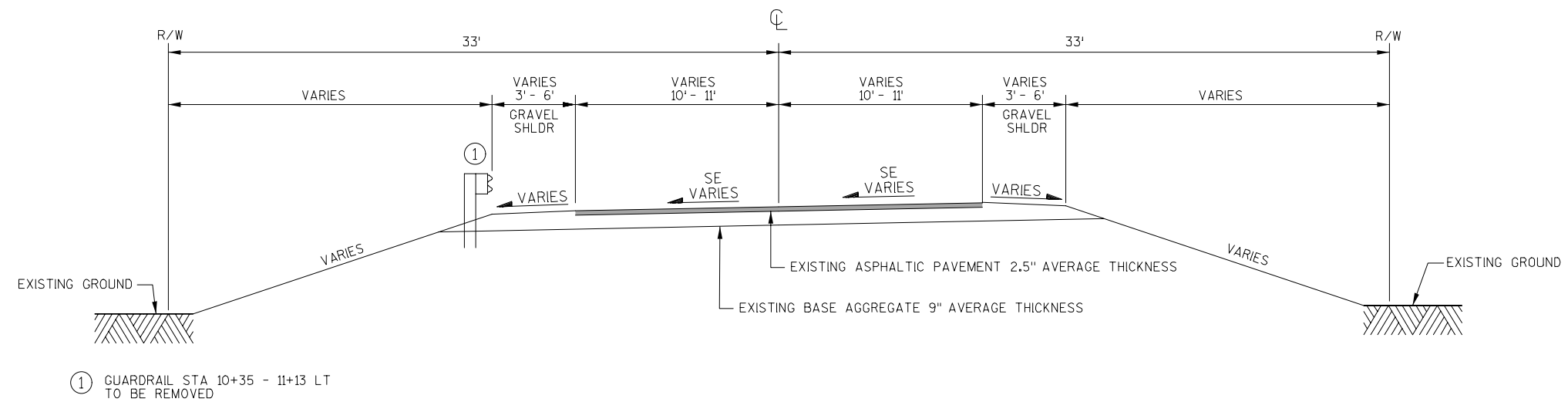
HWY: CTH V

COUNTY: RUSK

GENERAL NOTES

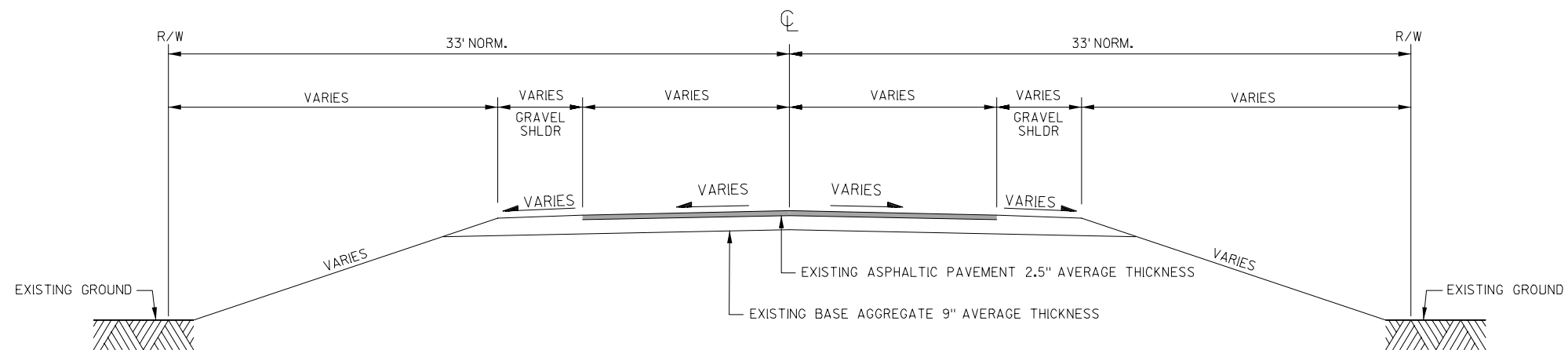
SHEET

E



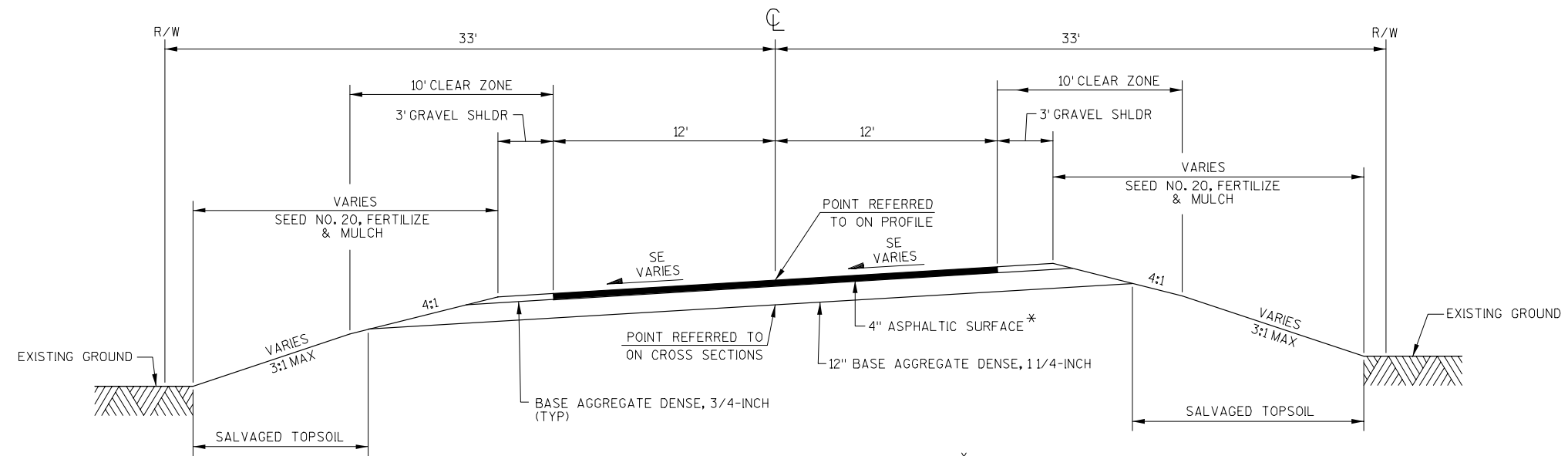
TYPICAL EXISTING SECTION - CTH V

STA 8+10.00 - STA 9+70.16
STA 10+30.24 - STA 11+95.00



TYPICAL EXISTING SECTION - CEMETERY ROAD

STA 49+00.00 - STA 50+00.00

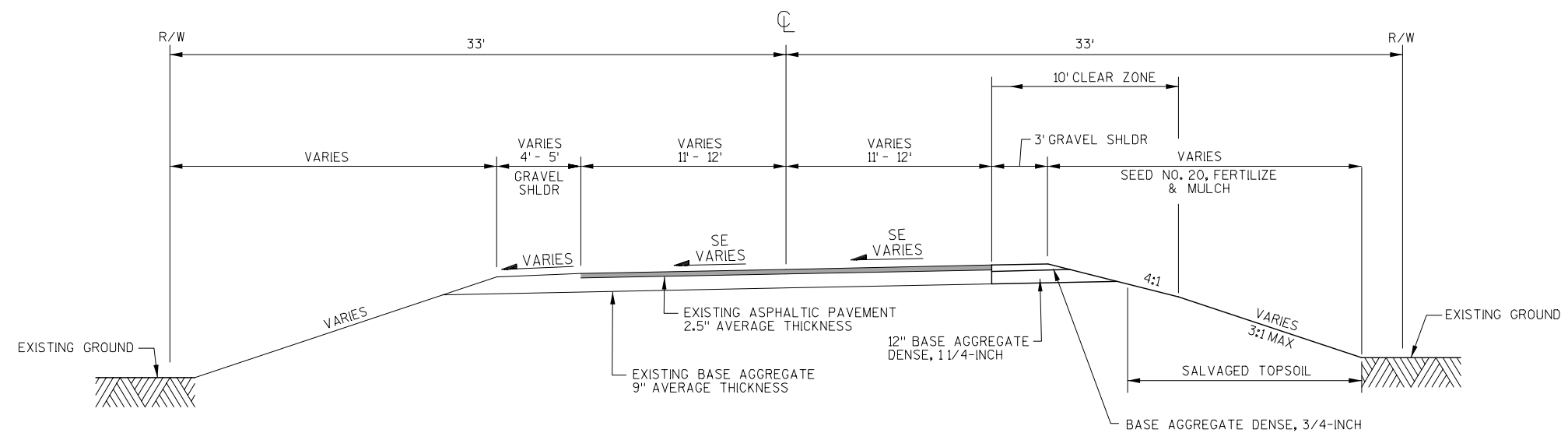


TYPICAL SUPERELEVATED FINISHED SECTION - CTH V

STA 8+10.00 - STA 9+60.41
STA 10+45.15 - STA 11+95.00

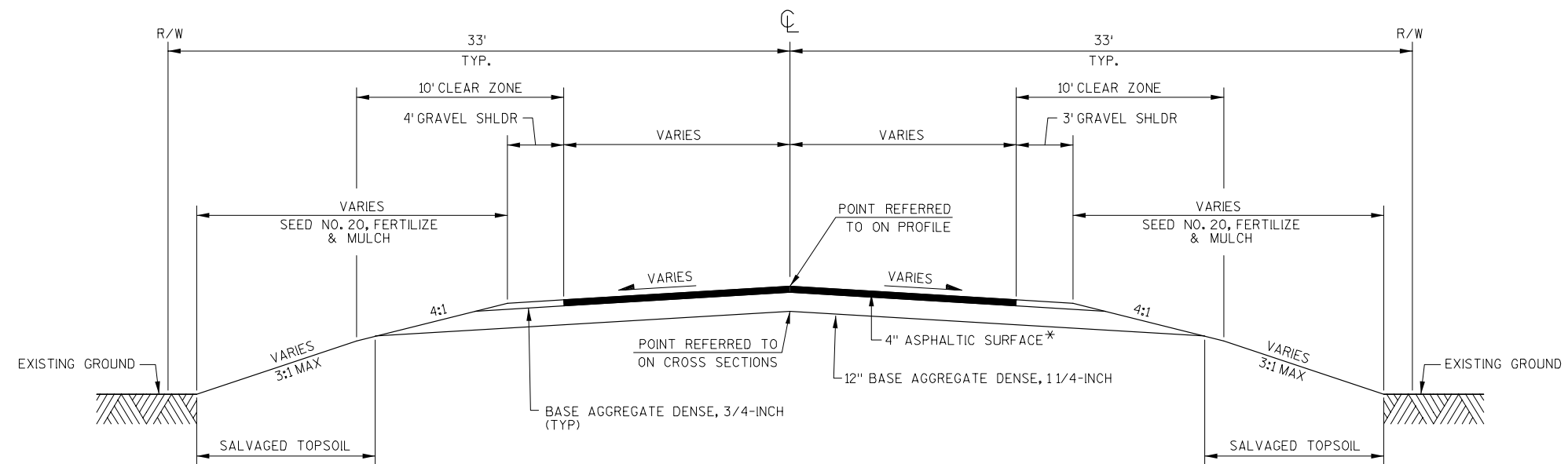
CTH V S.E. DATA

8+10	4.70% (WB)	-1.50% (EB)	MATCH EXISTING
8+51	6.00% (WB)	-6.00% (EB)	FULL SUPER
11+83	6.00% (WB)	-6.00% (EB)	FULL SUPER
11+95	7.10% (WB)	-5.10% (EB)	MATCH EXISTING



TYPICAL SUPERELEVATED FINISHED SECTION - CTH V

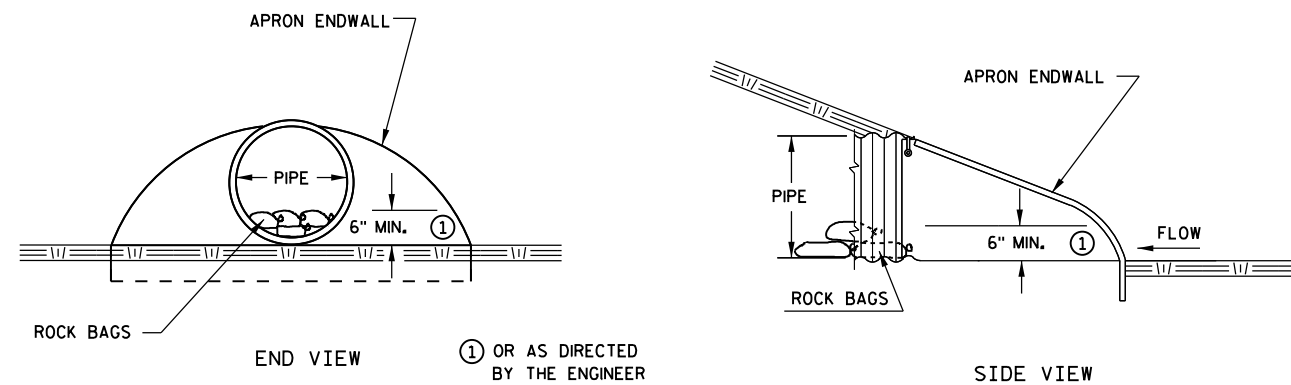
STA 11+95.00 - STA 12+50.00



* IF ASPHALTIC SURFACE IS TO BE PLACED IN TWO LIFTS USE
1.75-INCH ASPHALTIC SURFACE (LOWER LAYER)
2.25-INCH ASPHALTIC SURFACE (UPPER LAYER)

TYPICAL FINISHED SECTION - CEMETERY ROAD

STA 49+00.00 - STA 50+00.00



CULVERT PIPE CHECK

NTS

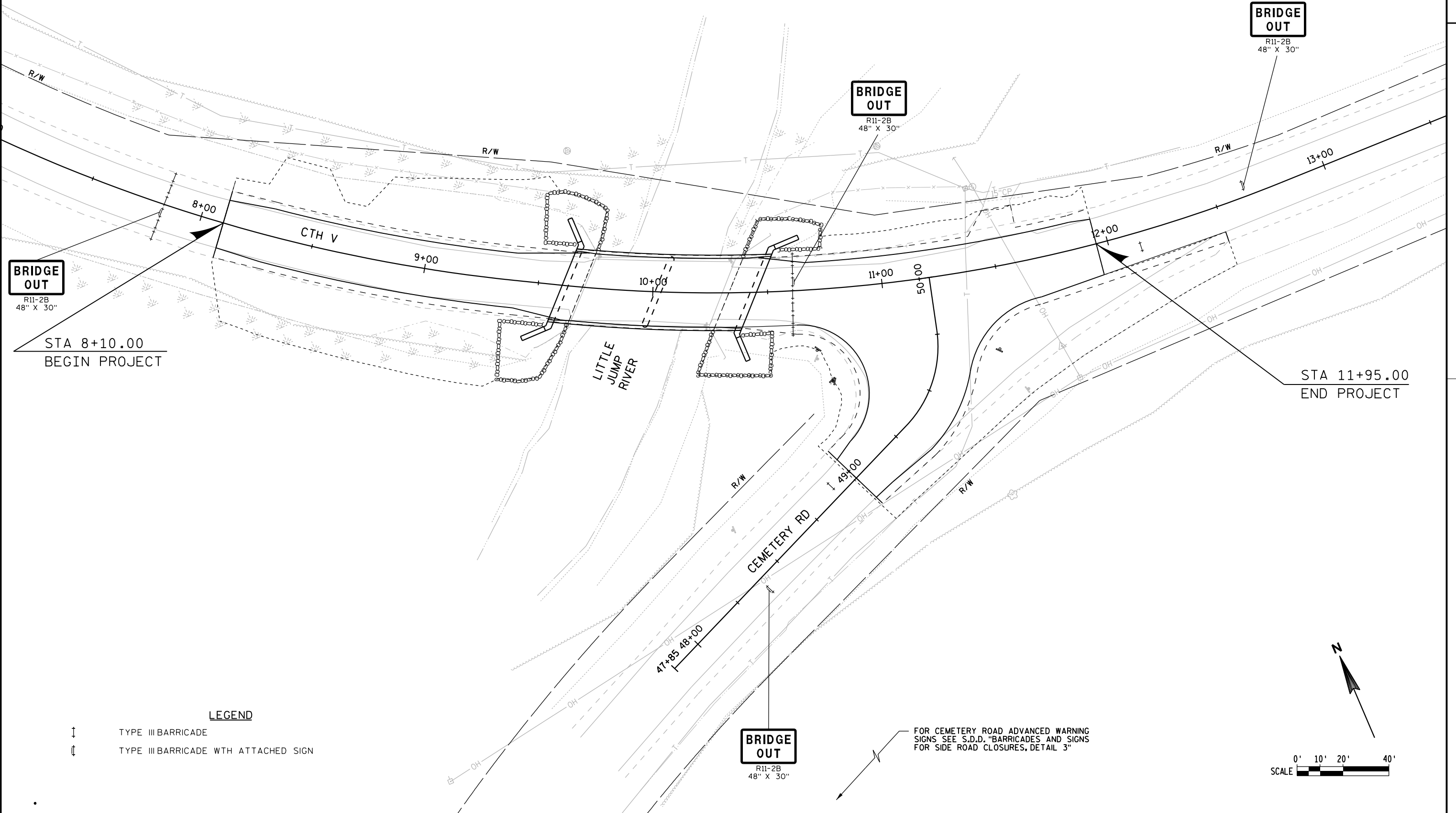


2

2

FOR CTH V CLOSURE
SEE S.D.D. "BARRICADES AND SIGNS
FOR MAINLINE CLOSURES, DETAIL C"

FOR CTH V CLOSURE
SEE S.D.D. "BARRICADES AND SIGNS
FOR MAINLINE CLOSURES, DETAIL C"



BRIDGE
OUT
R11-2B
48" X 30"

BRIDGE
OUT
R11-2B
48" X 30"

BRIDGE
OUT
R11-2B
48" X 30"

BRIDGE
OUT
R11-2B
48" X 30"

FOR CEMETERY ROAD ADVANCED WARNING
SIGNS SEE S.D.D. "BARRICADES AND SIGNS
FOR SIDE ROAD CLOSURES, DETAIL 3"

LEGEND

- ↑ TYPE III BARRICADE
- ↑↓ TYPE III BARRICADE WITH ATTACHED SIGN

PROJECT NUMBER: 8796-00-70

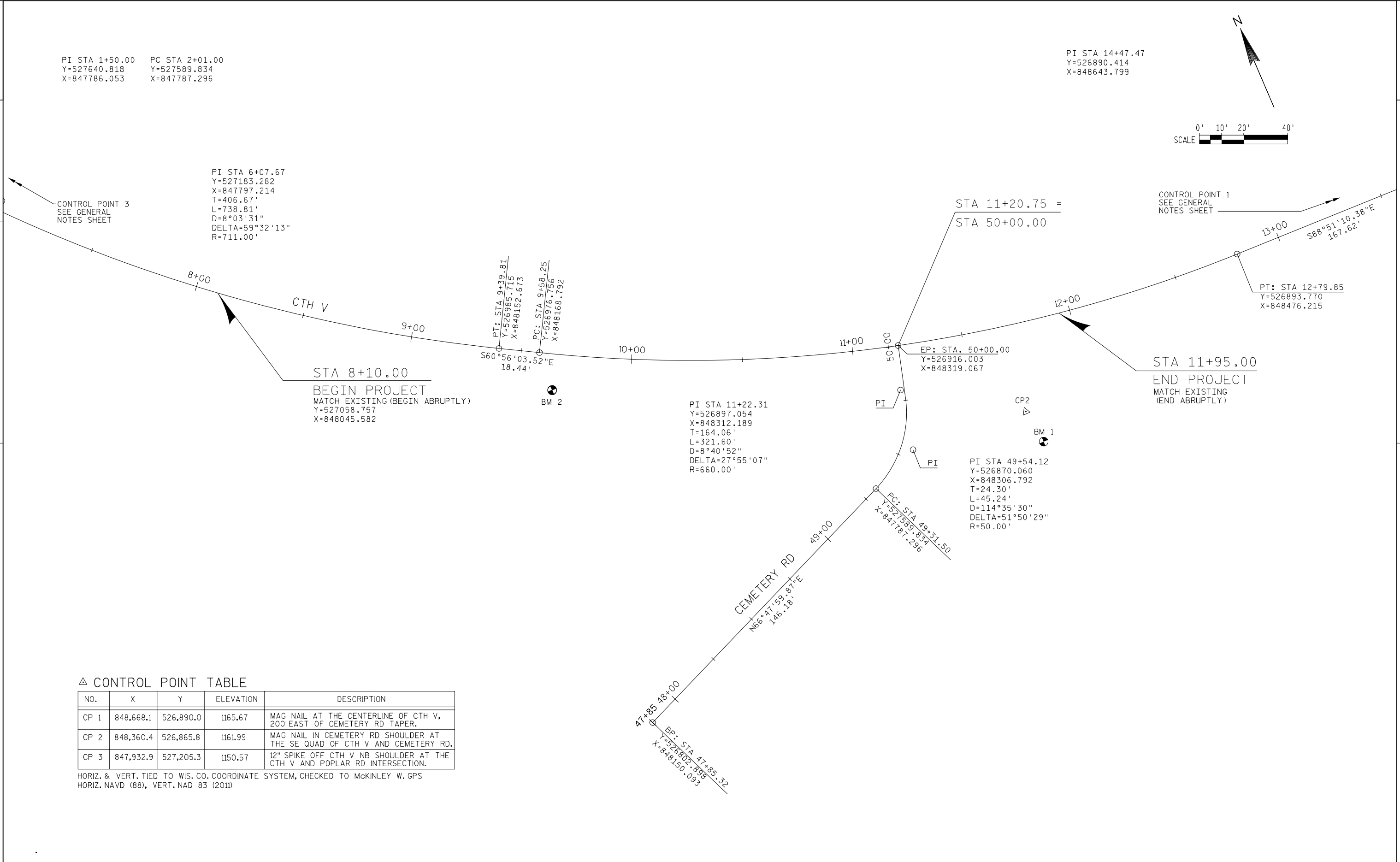
HWY: CTH V

COUNTY: RUSK

TRAFFIC CONTROL

SHEET

E



△ CONTROL POINT TABLE

NO.	X	Y	ELEVATION	DESCRIPTION
CP 1	848,668.1	526,890.0	1165.67	MAG NAIL AT THE CENTERLINE OF CTH V, 200' EAST OF CEMETERY RD TAPER.
CP 2	848,360.4	526,865.8	1161.99	MAG NAIL IN CEMETERY RD SHOULDER AT THE SE QUAD OF CTH V AND CEMETERY RD.
CP 3	847,932.9	527,205.3	1150.57	12" SPIKE OFF CTH V NB SHOULDER AT THE CTH V AND POPLAR RD INTERSECTION.

HORIZ. & VERT. TIED TO WIS. CO. COORDINATE SYSTEM, CHECKED TO MCKINLEY W. GPS
HORIZ. NAVD (88), VERT. NAD 83 (2011)

Estimate Of Quantities

8796-00-70

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	2.000	2.000
0004	201.0205	Grubbing	STA	2.000	2.000
0006	203.0600.S	Removing Old Structure Over Waterway With Minimal Debris (station) 01. 10+02.28	LS	1.000	1.000
0008	204.0165	Removing Guardrail	LF	76.000	76.000
0010	205.0100	Excavation Common	CY	744.000	744.000
0012	206.1000	Excavation for Structures Bridges (structure) 01. B-54-125	LS	1.000	1.000
0014	210.1500	Backfill Structure Type A	TON	740.000	740.000
0016	213.0100	Finishing Roadway (project) 01. 8796-00-70	EACH	1.000	1.000
0018	305.0110	Base Aggregate Dense 3/4-Inch	TON	75.000	75.000
0020	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,194.000	1,194.000
0022	465.0105	Asphaltic Surface	TON	208.000	208.000
0024	502.0100	Concrete Masonry Bridges	CY	365.000	365.000
0026	502.3200	Protective Surface Treatment	SY	285.000	285.000
0028	502.3210	Pigmented Surface Sealer	SY	75.000	75.000
0030	505.0400	Bar Steel Reinforcement HS Structures	LB	8,200.000	8,200.000
0032	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	54,850.000	54,850.000
0034	516.0500	Rubberized Membrane Waterproofing	SY	14.000	14.000
0036	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	690.000	690.000
0038	606.0300	Riprap Heavy	CY	250.000	250.000
0040	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	200.000	200.000
0042	614.0150	Anchor Assemblies for Steel Plate Beam Guard	EACH	4.000	4.000
0044	619.1000	Mobilization	EACH	1.000	1.000
0046	624.0100	Water	MGAL	95.500	95.500
0048	625.0500	Salvaged Topsoil	SY	1,288.000	1,288.000
0050	627.0200	Mulching	SY	1,466.000	1,466.000
0052	628.1504	Silt Fence	LF	710.000	710.000
0054	628.1520	Silt Fence Maintenance	LF	710.000	710.000
0056	628.1905	Mobilizations Erosion Control	EACH	1.000	1.000
0058	628.1910	Mobilizations Emergency Erosion Control	EACH	1.000	1.000
0060	628.6005	Turbidity Barriers	SY	348.000	348.000
0062	628.7504	Temporary Ditch Checks	LF	240.000	240.000
0064	628.7555	Culvert Pipe Checks	EACH	1.000	1.000
0066	628.7560	Tracking Pads	EACH	2.000	2.000
0068	629.0210	Fertilizer Type B	CWT	0.900	0.900
0070	630.0120	Seeding Mixture No. 20	LB	28.000	28.000
0072	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	4.000	4.000
0074	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	3.000	3.000
0076	637.2210	Signs Type II Reflective H	SF	9.000	9.000

Estimate Of Quantities

8796-00-70					
Line	Item	Item Description	Unit	Total	Qty
0078	637.2230	Signs Type II Reflective F	SF	39.500	39.500
0080	638.2602	Removing Signs Type II	EACH	6.000	6.000
0082	638.3000	Removing Small Sign Supports	EACH	6.000	6.000
0084	642.5001	Field Office Type B	EACH	1.000	1.000
0086	643.0100	Traffic Control (project) 01. 8796-00-70	EACH	1.000	1.000
0088	643.0420	Traffic Control Barricades Type III	DAY	1,580.000	1,580.000
0090	643.0705	Traffic Control Warning Lights Type A	DAY	2,054.000	2,054.000
0092	643.0900	Traffic Control Signs	DAY	1,264.000	1,264.000
0094	645.0111	Geotextile Type DF Schedule A	SY	112.000	112.000
0096	645.0120	Geotextile Type HR	SY	445.000	445.000
0098	646.0106	Pavement Marking Epoxy 4-Inch	LF	1,635.000	1,635.000
0100	647.0566	Pavement Marking Stop Line Epoxy 18-Inch	LF	19.000	19.000
0102	650.4500	Construction Staking Subgrade	LF	441.000	441.000
0104	650.5000	Construction Staking Base	LF	441.000	441.000
0106	650.6500	Construction Staking Structure Layout (structure) 01. B-54-125	LS	1.000	1.000
0108	650.9910	Construction Staking Supplemental Control (project) 01. 8796-00-70	LS	1.000	1.000
0110	650.9920	Construction Staking Slope Stakes	LF	441.000	441.000
0112	690.0150	Sawing Asphalt	LF	71.000	71.000
0114	715.0502	Incentive Strength Concrete Structures	DOL	2,190.000	2,190.000
0116	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	1,200.000	1,200.000
0118	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000

3

CLEARING AND GRUBBING					
LOCATION	STATION - STATION	201.0105 CLEARING		201.0205 GRUBBING	
		STA		STA	
CTH V	8+15 - 9+50	2		2	
PROJECT TOTALS		2		2	

REMOVING GUARDRAIL			
LOCATION	STATION - STATION	204.0165 REMOVING GUARDRAIL	
		LF	
CTH V - EAST OF BRIDGE	10+35 - 11+13	76	
PROJECT TOTAL		76	

3

EARTHWORK SUMMARY

		205.0100 EXCAVATION COMMON (1)	AVAILABLE MATERIAL (5)	FILL (6)	MASS ORDINATE +/- (8)	624.0100**
		CUT (2)		FACTOR 1.0		WATER
LOCATION	STATION - STATION	CY	CY	CY	CY	MGAL
CTH V - WEST OF BRIDGE	8+10.00 - 9+66.61	234	234	334	-100	0.5
CTH V - EAST OF BRIDGE	10+38.55 - 12+50.00	322	322	13	308	0.6
CEMETERY ROAD	49+00.00 - 49+50.00	188	188	4	184	0.4
PROJECT TOTALS		744	744	352	392	1.5
TOTAL EXCAVATION COMMON		744				

- 1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100.
- 2) SALVAGED/UNSUALE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- 5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUALE PAVEMENT MATERIAL.
- 6) FILL. FACTOR = 1.0.
- 8) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION. MASS ORDINATE = AVAILABLE MATERIAL - FILL. THE MASS ORDINATE IS FOR INFORMATION PURPOSES ONLY AS COMMON EXCAVATION AND ROADWAY EMBANKMENT ARE NOT BALANCED FOR QUANTITY PURPOSES AND DOES NOT GUARANTEE THE QUALITY OF COMMON EXCAVATION, AND IF IT CAN BE REUSED ONSITE. ALL EBS EXCAVATION MATERIAL IS ASSUMED TO BE WASTED OFFSITE UNLESS

*ALL ITEMS CATEGORY 0010 UNLESS OTHERWISE NOTED.
**ADDITIONAL QUANTITY LISTED ELSEWHERE

3

BASE AGGREGATE				
LOCATION	STATION - STATION	305.0120	305.0110	624.0100**
		BASE AGGREGATE	BASE AGGREGATE	WATER
		DENSE 1 1/4-INCH	DENSE 3/4-INCH	
		TON	TON	MGAL
CTH V - WEST OF BRIDGE	8+10 - 9+65	413	29	31
CTH V - EAST OF BRIDGE	10+39 - 11+95	780	46	59
PROJECT TOTALS		1,194	75	90

ASPHALTIC SURFACE			
LOCATION	STATION - STATION	SF	465.0105
			ASPHALTIC SURFACE
			TON
CTH V - WEST OF BRIDGE	8+10 - 9+65	3,636	68
CTH V - EAST OF BRIDGE	10+39 - 11+95	7,522	140
PROJECT TOTAL			208

3

<u>FINISHING ROADWAY</u>	
	213.0100
	FINISHING ROADWAY
PROJECT ID	EACH
8796-00-70	1
PROJECT TOTAL	1

FINISHING ITEMS						
LOCATION	STATION - STATION	625.0500	627.0200	629.0210	630.0120	624.0100**
		SALVAGED	MULCHING	FERTILIZER	SEEDING	WATER
		TOPSOIL		TYPE B	MIXTURE NO.20	
		SY	SY	CWT	LB	MGAL
CTH V - WEST OF BRIDGE	8+10 - 9+65	655	727	0.5	14	2
CTH V - EAST OF BRIDGE	10+39 - 11+95	375	445	0.3	8	1
UNDISTRIBUTED		258	293	0.2	6	1
PROJECT TOTALS		1,288	1,466	0.9	28	4

* ALL ITEMS CATEGORY 0010 UNLESS OTHERWISE NOTED.
** ADDITIONAL QUANTITIES LISTED ELSEWHERE.

MOBILIZATION	
PROJECT ID	619.1000 EACH
8796-00-70	1.0
<hr/>	
PROJECT TOTAL	1.0

EROSION CONTROL							
LOCATION	STATION	628.1504	628.1520	628.6005	628.7504	628.7555	628.7560
		SILT FENCE LF	SILT FENCE MAINTENANCE LF	TURBIDITY BARRIERS SY	TEMPORARY DITCH CHECKS LF	CULVERT PIPE CHECKS EACH	TRACKING PADS EACH
CTH V - WEST OF BRIDGE	8+10 - 10+00	315	315	122	-	-	1
CTH V - BRIDGE	9+90 - 10+10	-	-	146	-	-	-
CTH V - EAST OF BRIDGE	10+00 - 12+50	250	250	79	190	1	1
UNDISTRIBUTED		145	145	-	50	-	-
PROJECT TOTALS		710	710	348	240	1	2

PERMANENT SIGNING

			637.2210			637.2230		634.0612		634.0616		638.2602		638.3000			
			SIGN SIZE			SIGN TYPE II REFLECTIVE H		SIGN TYPE II REFLECTIVE F		POSTS WOOD 4X6- INCH 12-FT		POSTS WOOD 4X6- INCH 16-FT		REMOVING SIGNS TYPE II		REMOVING SMALL SIGN SUPPORTS	
SIGN #	SIGN CODE	SIGN TYPE	INCHES			SF	SF	EACH		EACH		EACH		EACH		REMARKS	
1-01	W5-52R	II	18	X	54	-	6.75	1		-		1		1		BRIDGE HASH MARKS	
1-02	W5-52L	II	18	X	54	-	6.75	1		-		1		1		BRIDGE HASH MARKS	
1-03	W5-52R	II	18	X	54	-	6.75	1		-		1		1		BRIDGE HASH MARKS	
1-04	W5-52L	II	18	X	54	-	6.75	1		-		1		1		BRIDGE HASH MARKS	
1-05	W1-6	II	60	X	30	-	12.50	-		2		1		1		RIGHT	
1-06	R1-1	II	36	X	36	9.00	-	-		1		1		1		STOP	
PROJECT TOTALS						9.00	39.50	4		3		6		6			

*ALL ITEMS CATEGORY 0010 UNLESS OTHERWISE NOTED.

3

3

<div><div>MOBILIZATIONS EROSION CONTROL</div><table><tr><td></td><td>628.1905</td><td>628.1910</td></tr><tr><td></td><td>MOBILIZATIONS</td><td>MOBILIZATIONS</td></tr><tr><td></td><td>EROSION CONTROL</td><td>EROSION CONTROL</td></tr><tr><td>PROJECT ID</td><td>EACH</td><td>EACH</td></tr><tr><td>8796-00-70</td><td>1</td><td>1</td></tr><tr><td>PROJECT TOTALS</td><td>1</td><td>1</td></tr></table></div>				628.1905	628.1910		MOBILIZATIONS	MOBILIZATIONS		EROSION CONTROL	EROSION CONTROL	PROJECT ID	EACH	EACH	8796-00-70	1	1	PROJECT TOTALS	1	1	<div><div>PERMANENT PAVEMENT MARKINGS</div><table><tr><td></td><td></td><td></td><td></td><td>646.0106</td><td></td><td>647.0566</td><td></td></tr><tr><td></td><td></td><td></td><td></td><td colspan="2">PAVEMENT MARKING</td><td colspan="2">PAVEMENT MARKING</td></tr><tr><td></td><td></td><td></td><td></td><td colspan="2">EPOXY 4-INCH</td><td colspan="2">STOP LINE</td></tr><tr><td></td><td></td><td></td><td></td><td>YELLOW</td><td>WHITE</td><td colspan="2">EPOXY 18-INCH</td></tr><tr><td>ROADWAY</td><td>STATION</td><td>-</td><td>STATION</td><td>LF</td><td>LF</td><td colspan="2">LF</td></tr><tr><td>CTH V - WEST OF BRIDGE</td><td>8+10</td><td>-</td><td>10+00</td><td>380</td><td>380</td><td colspan="2">-</td></tr><tr><td>CTH V - EAST OF BRIDGE</td><td>10+00</td><td>-</td><td>12+50</td><td>390</td><td>343</td><td colspan="2">-</td></tr><tr><td>CEMETERY RD</td><td>49+00</td><td>-</td><td>49+72</td><td>142</td><td>-</td><td colspan="2">19</td></tr><tr><td>PROJECT TOTALS</td><td></td><td></td><td></td><td colspan="2">1,635</td><td colspan="2">19</td></tr></table></div>											646.0106		647.0566						PAVEMENT MARKING		PAVEMENT MARKING						EPOXY 4-INCH		STOP LINE						YELLOW	WHITE	EPOXY 18-INCH		ROADWAY	STATION	-	STATION	LF	LF	LF		CTH V - WEST OF BRIDGE	8+10	-	10+00	380	380	-		CTH V - EAST OF BRIDGE	10+00	-	12+50	390	343	-		CEMETERY RD	49+00	-	49+72	142	-	19		PROJECT TOTALS				1,635		19																									
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<div><div>FIELD OFFICE</div><table><tr><td></td><td>642.5001</td></tr><tr><td></td><td>FIELD OFFICE</td></tr><tr><td></td><td>TYPE B</td></tr><tr><td>PROJECT ID</td><td>EACH</td></tr><tr><td>8796-00-70</td><td>1</td></tr><tr><td>PROJECT TOTAL</td><td>1</td></tr></table></div>			642.5001		FIELD OFFICE		TYPE B	PROJECT ID	EACH	8796-00-70	1	PROJECT TOTAL	1	<div><div>TRAFFIC CONTROL PROJECT</div><table><tr><td></td><td>643.0100</td></tr><tr><td></td><td>TRAFFIC</td></tr><tr><td></td><td>CONTROL</td></tr><tr><td>PROJECT ID</td><td>8796-00-70</td></tr><tr><td></td><td>EACH</td></tr><tr><td>8796-00-70</td><td>1</td></tr><tr><td>PROJECT TOTAL</td><td>1</td></tr></table></div>			643.0100		TRAFFIC		CONTROL	PROJECT ID	8796-00-70		EACH	8796-00-70	1	PROJECT TOTAL	1	<div><div>TRAFFIC CONTROL ITEMS</div><table><tr><td></td><td></td><td>643.0420</td><td></td><td>643.0705</td><td></td><td>643.0900</td><td></td></tr><tr><td></td><td></td><td>TRAFFIC CONTROL</td><td></td><td>TRAFFIC CONTROL</td><td></td><td>TRAFFIC CONTROL</td><td></td></tr><tr><td></td><td></td><td>BARRICADES</td><td></td><td>WARNING LIGHTS</td><td></td><td>SIGNS</td><td></td></tr><tr><td></td><td></td><td>TYPE III</td><td></td><td>TYPE A</td><td></td><td></td><td></td></tr><tr><td>PROJECT ID</td><td>ESTIMATED</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td>DURATION</td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td>DAYS</td><td>EACH</td><td>DAYS</td><td>EACH</td><td>DAYS</td><td>EACH</td><td>DAYS</td></tr><tr><td>8796-00-70</td><td>79</td><td>20</td><td>1,580</td><td>26</td><td>2,054</td><td>16</td><td>1,264</td></tr><tr><td>PROJECT TOTALS</td><td></td><td></td><td>1,580</td><td></td><td>2,054</td><td></td><td>1,264</td></tr></table></div>								643.0420		643.0705		643.0900				TRAFFIC CONTROL		TRAFFIC CONTROL		TRAFFIC CONTROL				BARRICADES		WARNING LIGHTS		SIGNS				TYPE III		TYPE A				PROJECT ID	ESTIMATED								DURATION								DAYS	EACH	DAYS	EACH	DAYS	EACH	DAYS	8796-00-70	79	20	1,580	26	2,054	16	1,264	PROJECT TOTALS			1,580		2,054		1,264																
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PROJECT NO: 8796-00-70		HWY: CTH V		COUNTY: RUSK		MISCELLANEOUS QUANTITIES			SHEET		E																																																																																																																

CONVENTIONAL SIGNS AND ABBREVIATIONS

STATE LINE	---	SECTION CORNER		FOUNDATION OR RUIN BUILDING	
COUNTY LINE	---				
TOWNSHIP AND RANGE LINES	---				
SECTION LINE	---	NOTATION FOR COMBUSTIBLE FLUIDS		CEMETERY	
QUARTER LINE	---			R/W MONUMENT	
SIXTEENTH LINE	---			NON-MONUMENTED R/W POINT	
NEW REFERENCE LINE	---	NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES		IRON PIN	
NEW R/W LINE	---			VALVE	
EXISTING R/W LINE	---			WINDMILL	
PROPERTY LINE	---	BRIDGE		MANHOLE, SEPTIC VENT, WELL, ETC.	
CORPORATE LIMITS	---			GAS PUMPS	
LOT, TIE AND OTHER MINOR LINES	---	STREAM OR RIVER		BUSHES	
SLOPE INTERCEPTS	---			TREES (Deciduous)	
UNDERGROUND FACILITY (Communications, Electric, Etc.)	---	LAKE		(Coniferous)	
FENCE	---	CULVERT (Box, Pipe Or Cattle Pass)		WOODS	
FEE INTEREST	---			ENCROACHING SIGN	
TEMPORARY INTEREST	---	SIGN	/ SIGN		
EASEMENT (Highway, Permanent Limited or Restricted Development)	---				
BEAM GUARD	---				
TRANSMISSION STRUCTURES (Line Optional)	---				
RAIL LINE	---				

ELECTRIC POLE		COMPENSABLE		NON-COMPENSABLE	
TELEPHONE POLE					
PEDESTAL (Label Type - Communications, Electric)					
NO ACCESS (By Acquisition)					
NO ACCESS (By Statutory Authority)					
NO ACCESS (By Previous Project)					

P.I.	Point of Intersection	ST.	Street
or PI		IP	Iron Pipe or Iron Pin
°.	Deflection Angle	C.S.M.	Certified Survey Map
D.	Degree of Curve	COR.	Corner
T.	Tangent Length	L.C.	Long Chord
L.	Length	L.C.B.	Long Chord Bearing
R.	Radius	MI.	Miles
CATV	Cable Television Line	MISC	Miscellaneous
FO	Fiber Optic Cable	N/A	Not Available or Applicable
G	Gas Line	P.L.	Property Line
GUY	Guy Wire	P.L.E.	Permanent Limited Easement
GV	Gas Valve	P.O.B.	Point of Beginning
SAN	Sanitary Sewer Line	PC	Point of Curvature
SEPV.	Septic Vent	PG.	Page
T	Telephone Line	PROP	Property Corner
W	Water Line	PT	Point of Tangency
ANT.	Antenna	R/W	Right of Way
B	Barn or Building	RD.	Road
G	Garage	REM.	Remnant
H	House	S.F.	Square Feet
S	Shed	SEC.	Section
C.T.H.	County Trunk Highway	STA.	Station
CORP	Corporation	T.L.E.	Temporary Limited Easement
LLC	Limited Liability Corporation	or TLE	
RR.	Railroad	VOL.	Volume
S.T.H.	State Trunk Highway		

NOTES

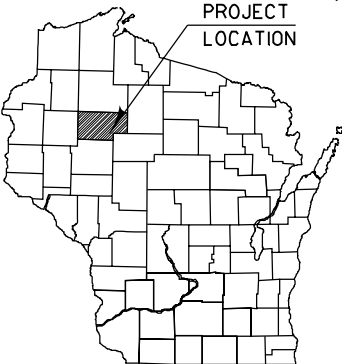
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATES REFERENCE SYSTEM COORDINATES (WISCRS), RUSK COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED FOR GROUND DISTANCES.

RIGHT OF WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY 3/4 " X 24" REBAR) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

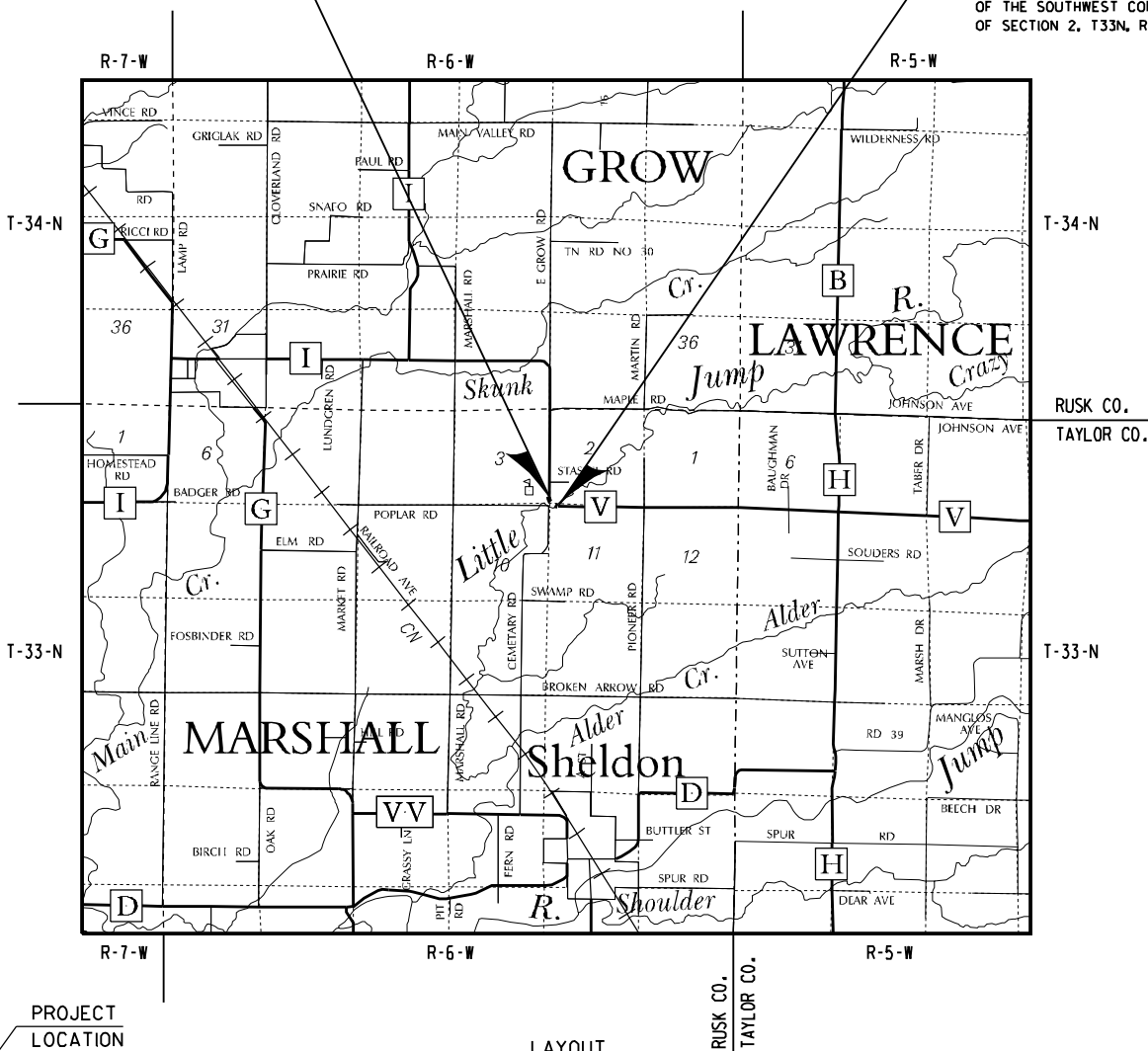
RIGHT OF WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE RUSK COUNTY COUNTY HIGHWAY DEPARTMENT.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. EXCLUDING RIGHTOF WAY LINES, THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.



BEGIN RELOCATION ORDER
STA. 7+75.00
199.08' NORTH AND 247.96' EAST
OF THE SOUTHWEST CORNER
OF SECTION 2, T33N, R5W.



LAYOUT
SCALE 0 MI.

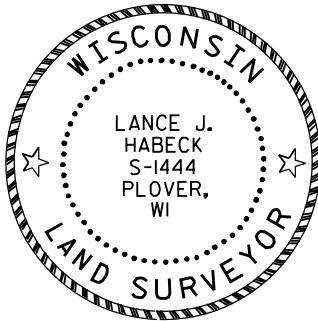
TOTAL NET LENGTH OF CENTERLINE = 0.085 MI.

END RELOCATION ORDER
STA. 12+25.00
14.54' NORTH AND 650.31' EAST
OF THE SOUTHWEST CORNER
OF SECTION 2, T33N, R5W.



ORIGINAL PLAT PREPARED BY

AECOM



Lance J. Habeck
LANCE J. HABECK, PLS-1444

DATE: 1/10/2017

REVISION DATE

RUSK COUNTY
HIGHWAY DEPARTMENT

APPROVED FOR THE DEPARTMENT

DATE: (Signature)

E

NOTE: EXISTING RIGHT OF WAY FOR CTH V WAS ESTABLISHED BY WARRANTY DEED, VOL. 165, PG. 330, DOC. 173388. THE EXISTING RIGHT OF WAY FOR CEMETERY ROAD WAS ESTABLISHED BY WARRANTY DEED, VOL. 173, PG. 809, DOC. 177429.

SCHEDULE OF LANDS & INTERESTS REQUIRED			
PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	TLE ACRES
1	ROBERT A. WIMER & LORA JEAN WIMER	FEE	0.13

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER (S)	INTEREST REQUIRED
50	CENTURYLINK (TELEPHONE)	RELEASE OF RIGHTS

R/W POINT COORDINATE TABLE		
POINT	Y	X
402	527103.310	848038.851
403	527027.089	848187.328
404	526950.400	848307.757
407	526919.364	848423.785

P.I. = 6+07.67
Y 527183.282
X 847797.214
Δ. = 59°32'13" LT.
D. = 08°03'31"
T. = 406.67'
L. = 738.81'
R. = 711.00'
P.C. = 2+01.00
B.K. = S01°23'51"E

P.I. = 11+22.31
Y 526897.054
X 848312.189
Δ. = 27°55'07" LT.
D. = 08°40'52"
T. = 164.06'
L. = 321.60'
R. = 660.00'

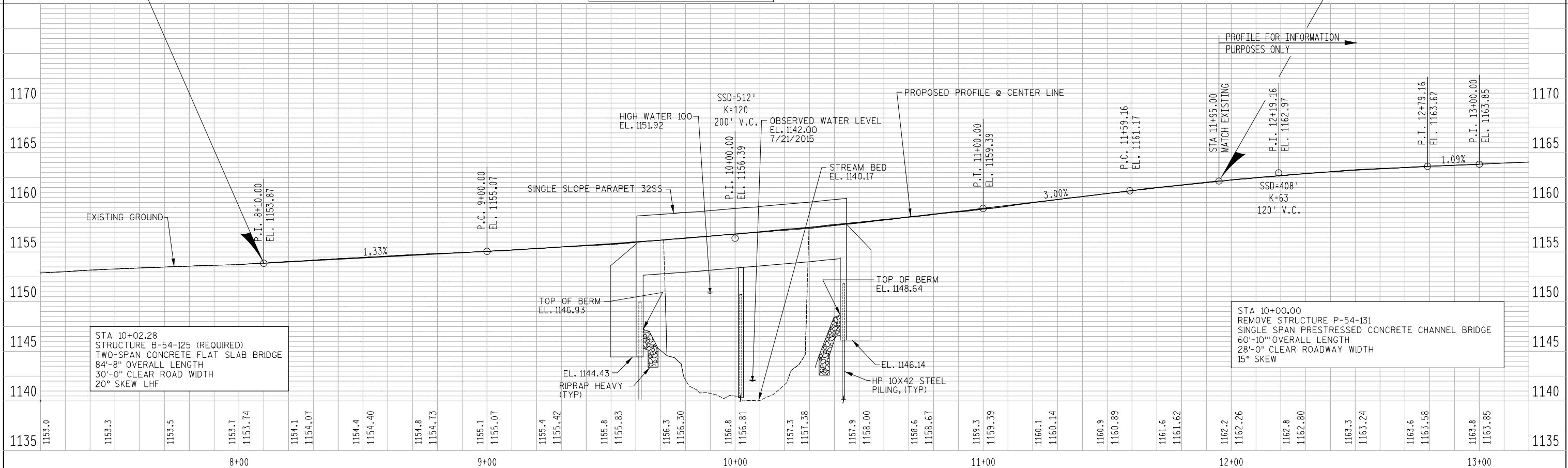
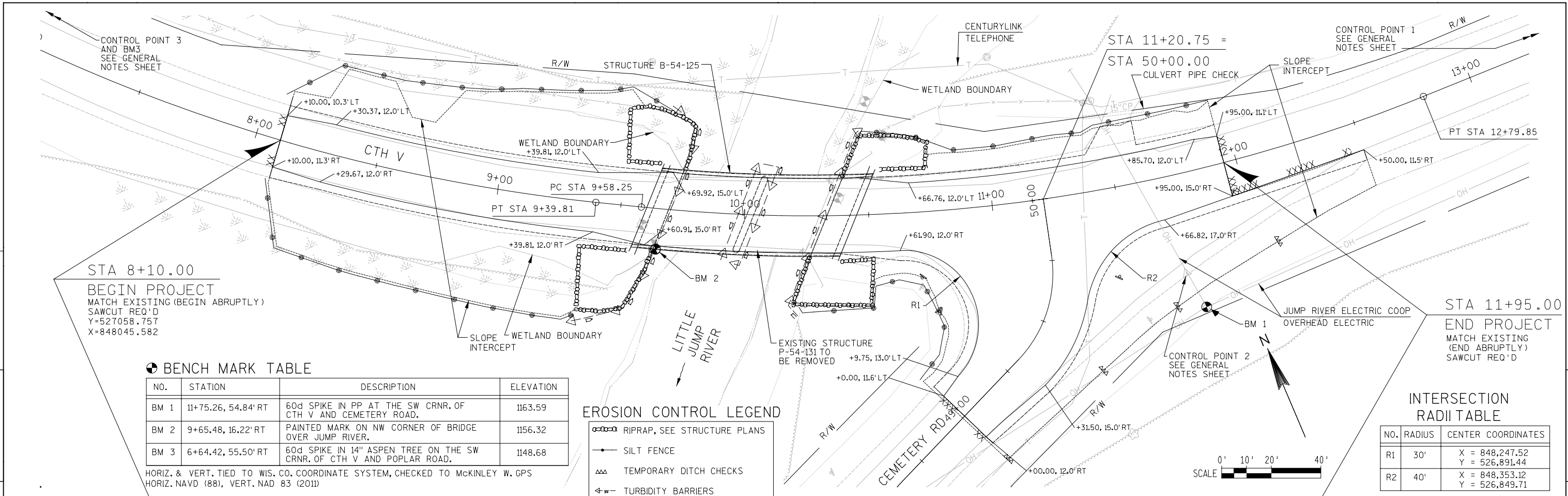
T 33 N
3 2
10 11
R 5 W

ALUMINUM MON.
FOUND
Y=526882.602
X=847771.180

T 33 N
2
11
R 5 W

SURVEY NAIL
FOUND
Y=526874.019
X=850424.069

REVISION DATE	DATE 1-10-17	SCALE, FEET 0 20 40	HWY: CTH V	STATE R/W PROJECT NUMBER 8796-00-00	PLAT SHEET 4.02	E
	GRID FACTOR N/A		COUNTY: RUSK	CONSTRUCTION PROJECT NUMBER 8796-00-70	PS&E SHEET	



EROSION CONTROL LEGEND

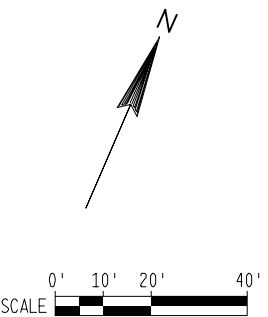
- RIPRAP, SEE STRUCTURE PLANS
- SILT FENCE
- △△ TEMPORARY DITCH CHECKS
- ←←← TURBIDITY BARRIERS

LITTLE
JUMP
RIVER

STRUCTURE B-54-125

CENTURYLINK
TELEPHONE

STA 11+20.75 =
STA 50+00.00



SLOPE INTERCEPT
R/W

CEMETERY RD

STA 49+00.00
END CONSTRUCTION

JUMP RIVER ELECTRIC COOP
OVERHEAD ELECTRIC

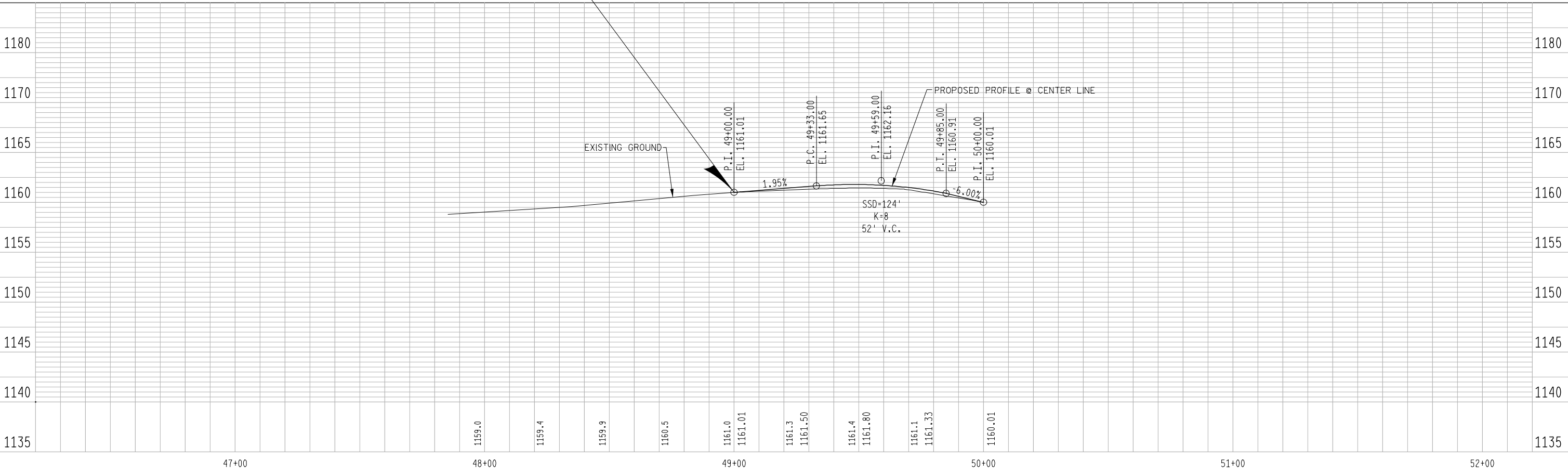
CULVERT PIPE CHECK

SLOPE INTERCEPT

CTH V

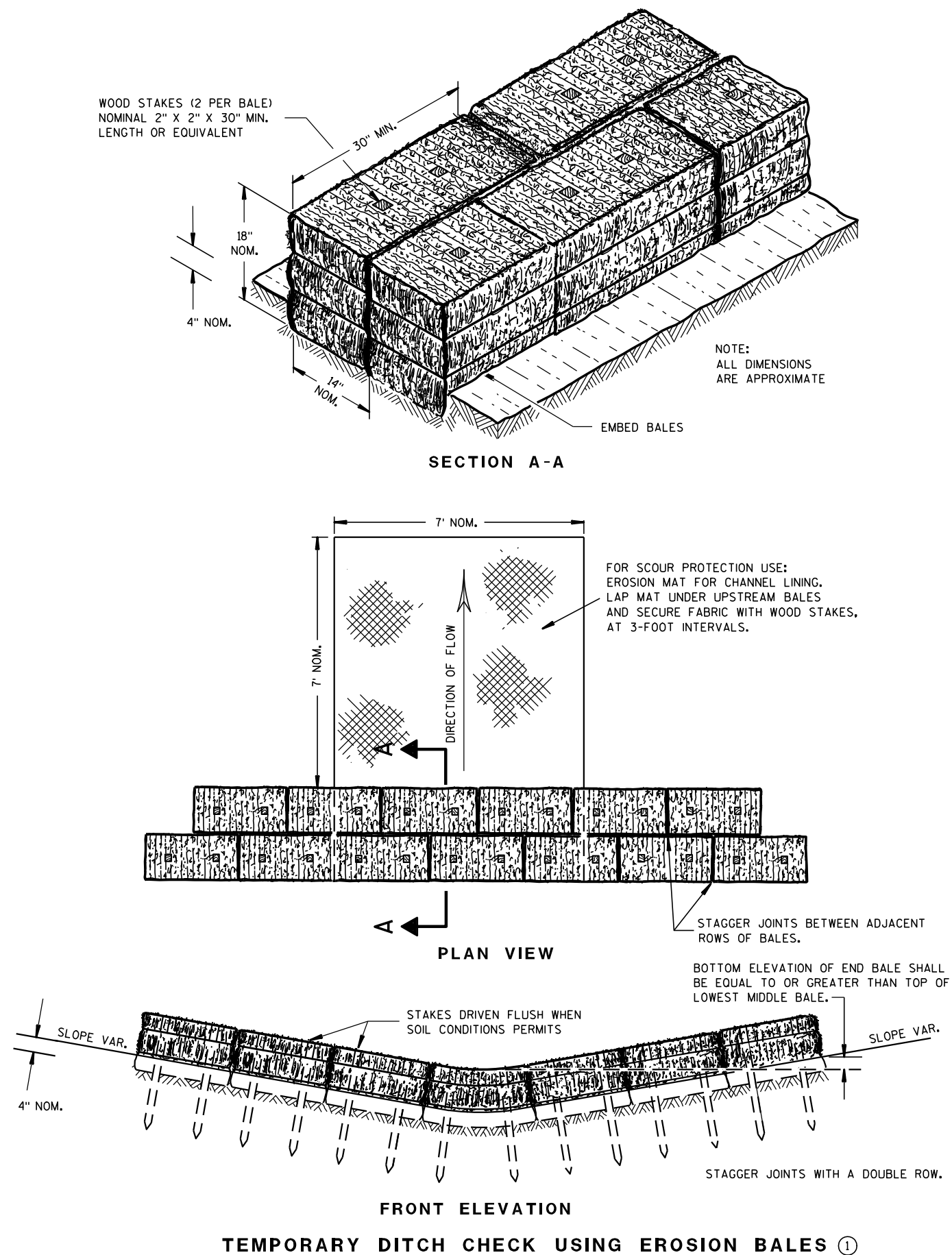
1180
1170
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1180
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1135



Standard Detail Drawing List

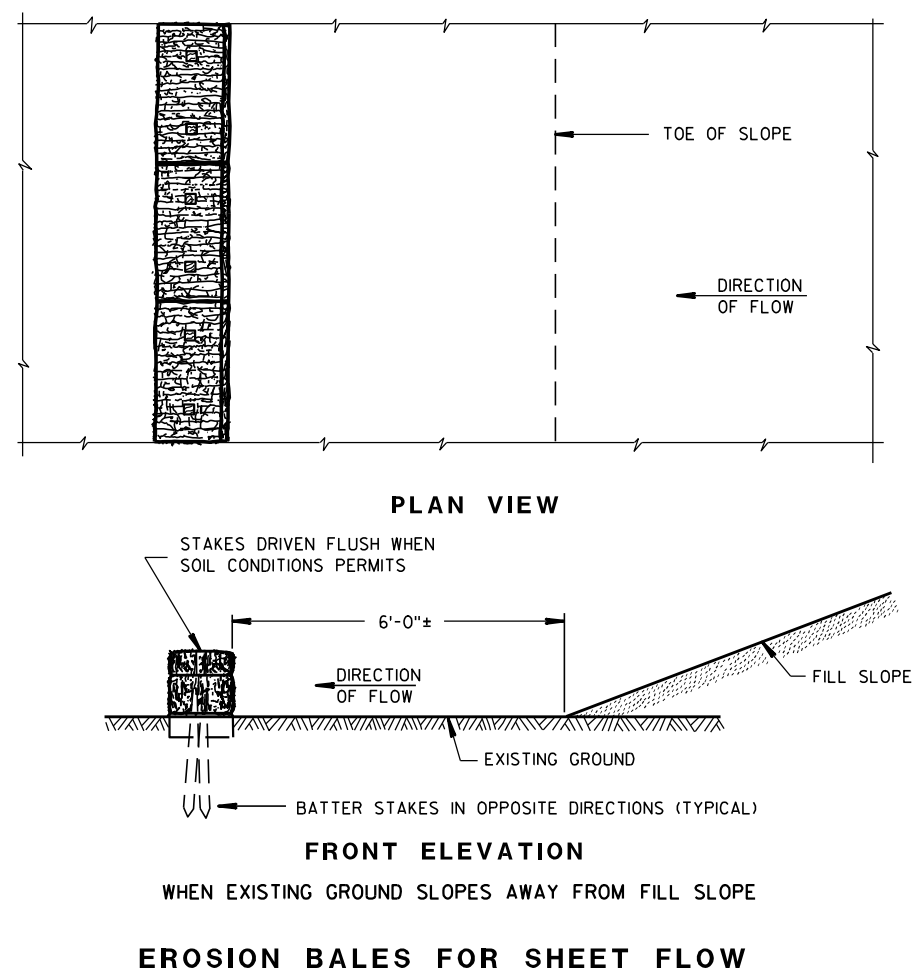
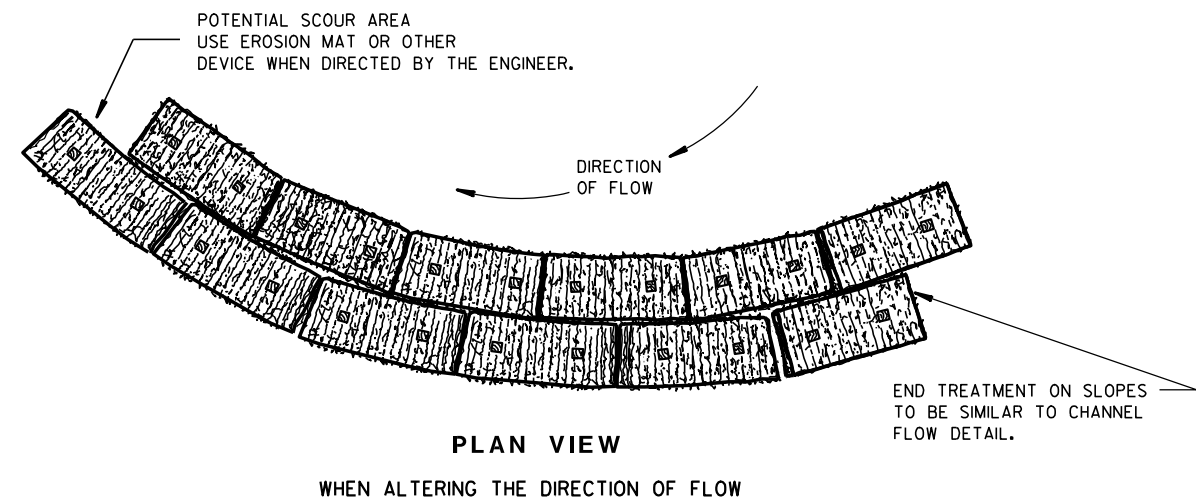
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
08E14-01	TRACKING PAD
12A03-10	NAME PLATE (STRUCTURES)
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C03-03	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C06-08	SIGNING & MARKING FOR TWO LANE BRIDGES
15C08-17A	PAVEMENT MARKING (MAINLINE)
15C08-17B	PAVEMENT MARKING (INTERSECTIONS)
15C33-02	STOP LINE AND CROSSWALK PAVEMENT MARKING



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

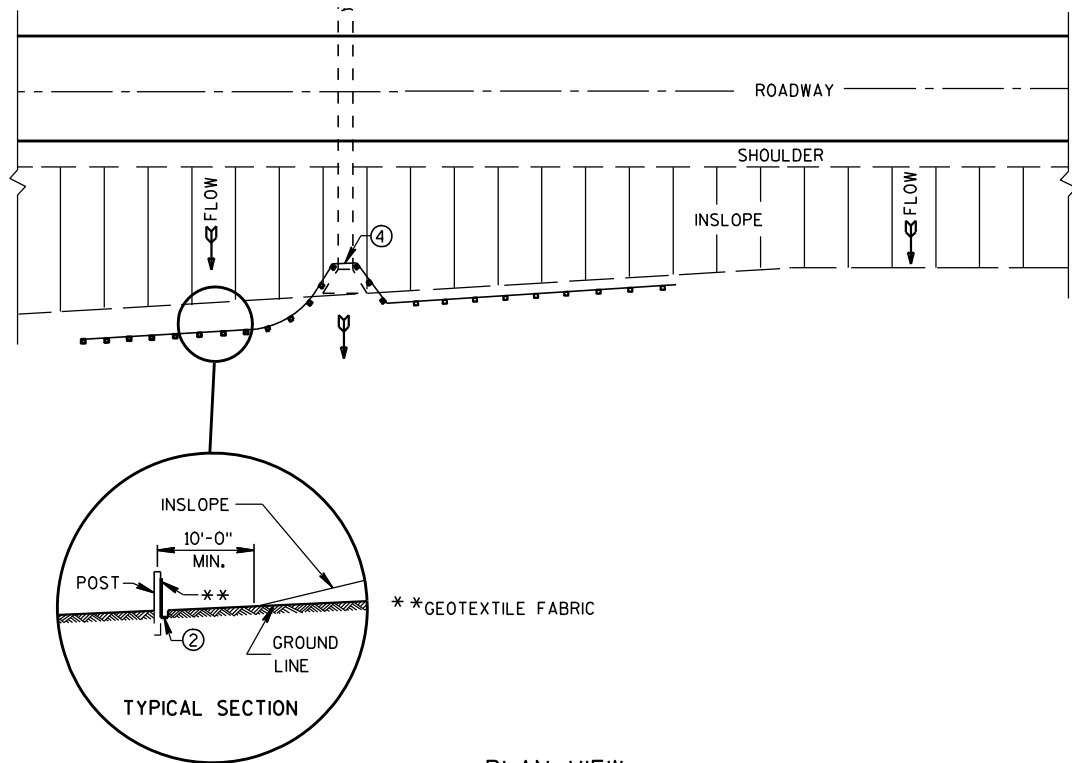
TYPICAL INSTALLATIONS OF
EROSION BALES / TEMPORARY
DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

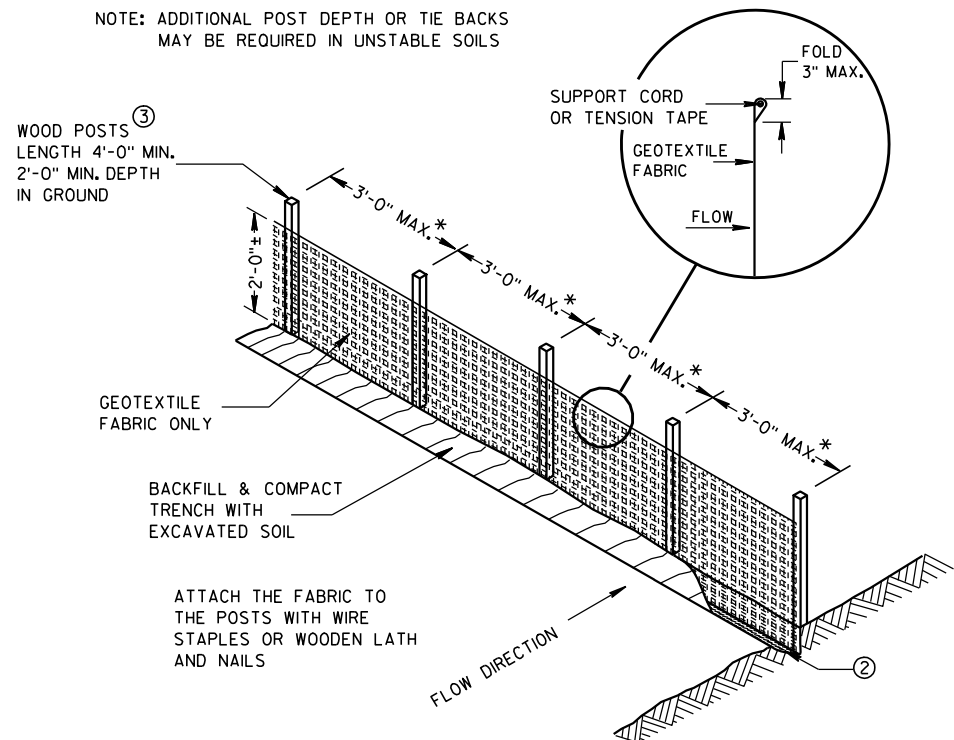
APPROVED

6/04/02
DATE/S/ Beth Canestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

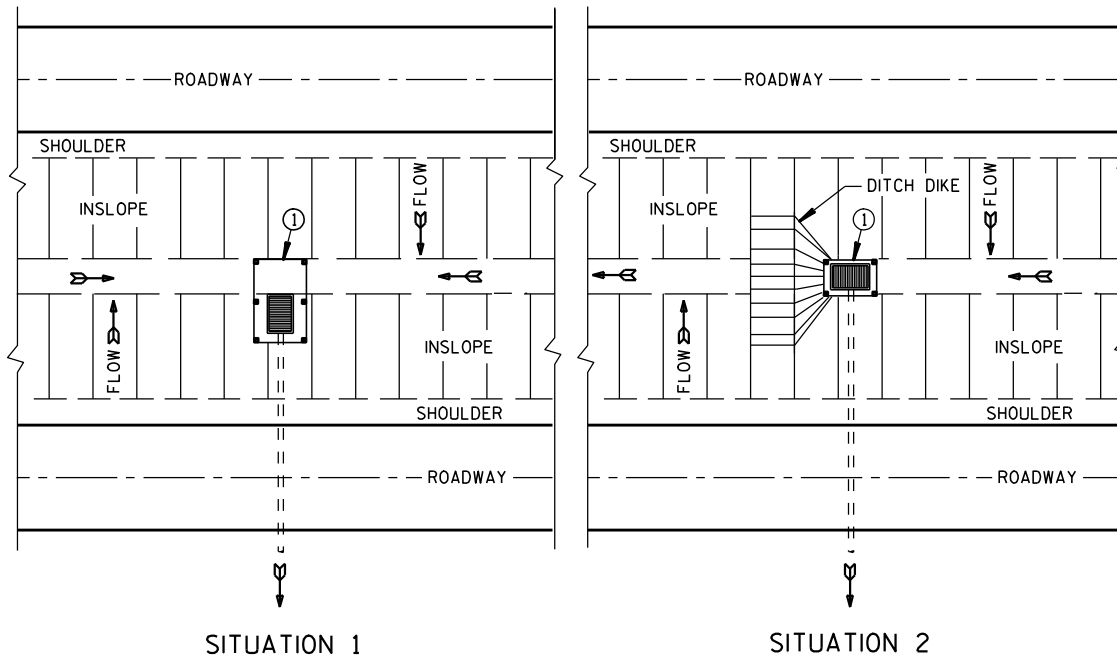
FHWA



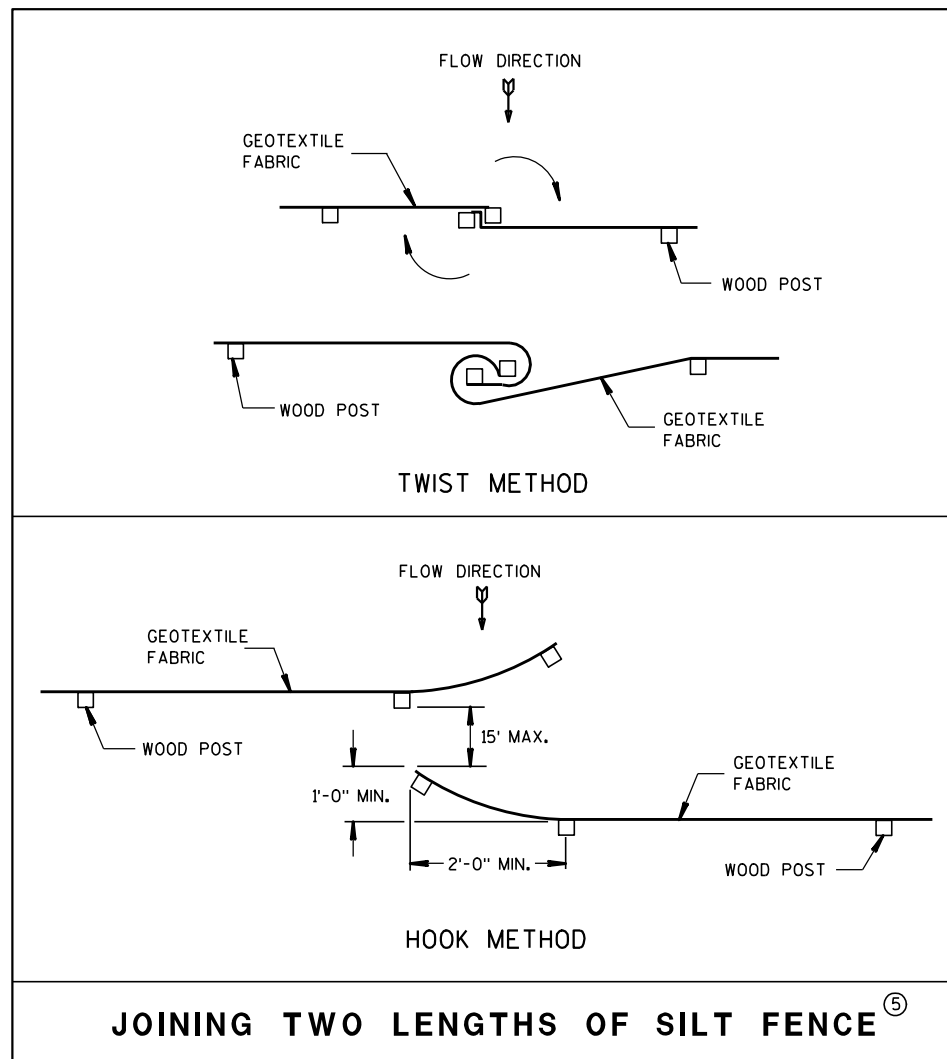
PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE



SILT FENCE



PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

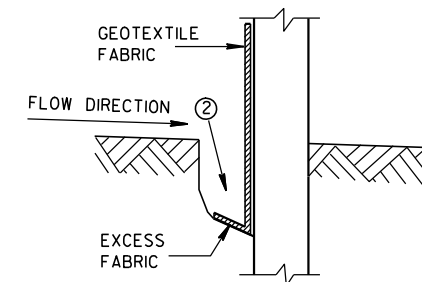


JOINING TWO LENGTHS OF SILT FENCE

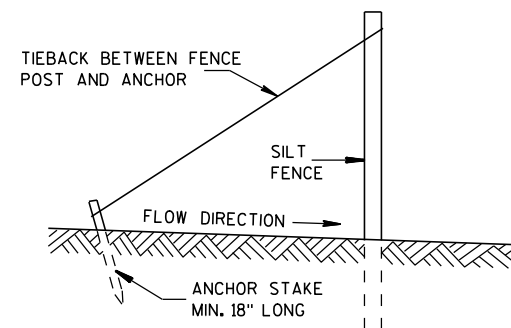
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

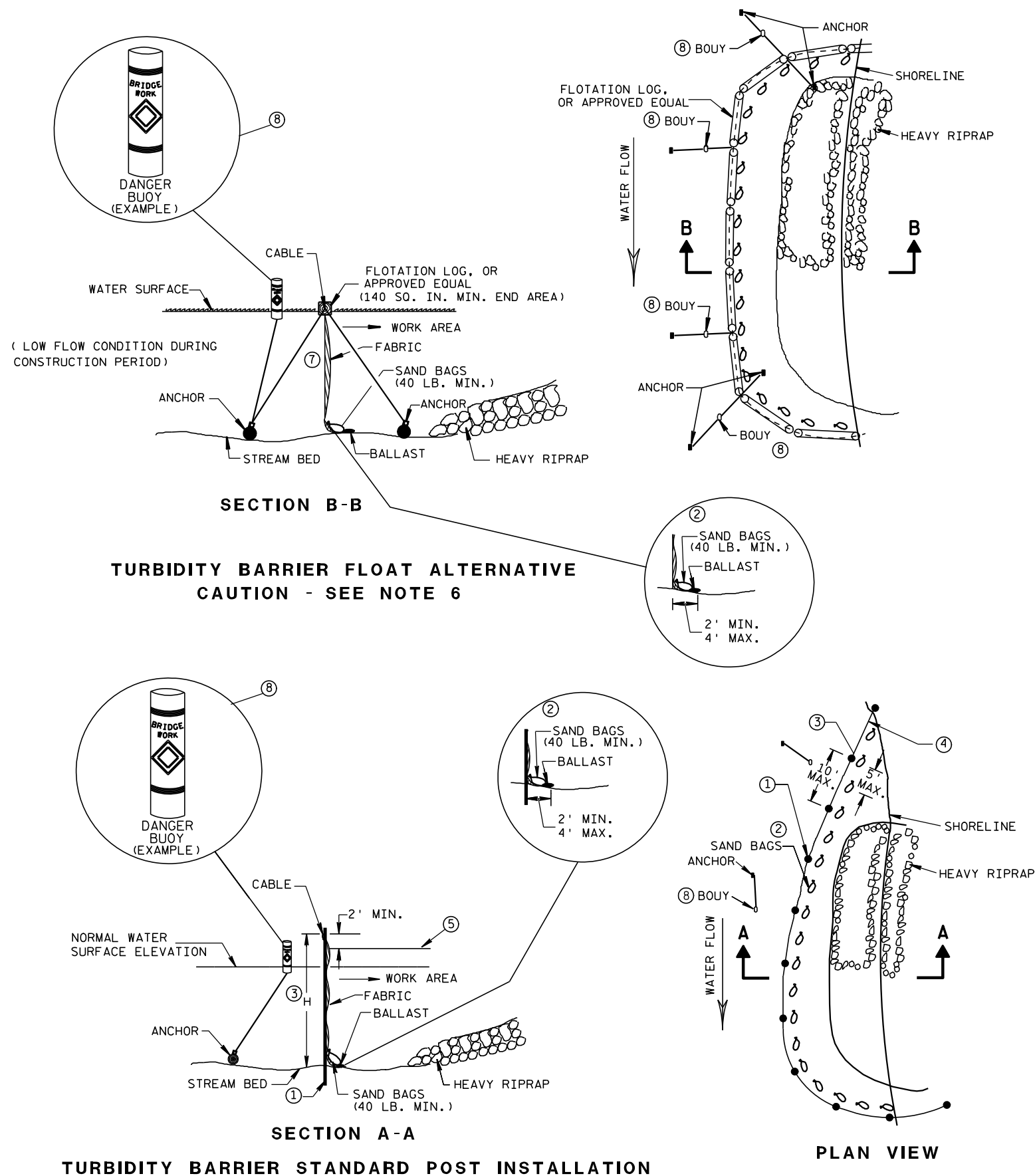


TRENCH DETAIL



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

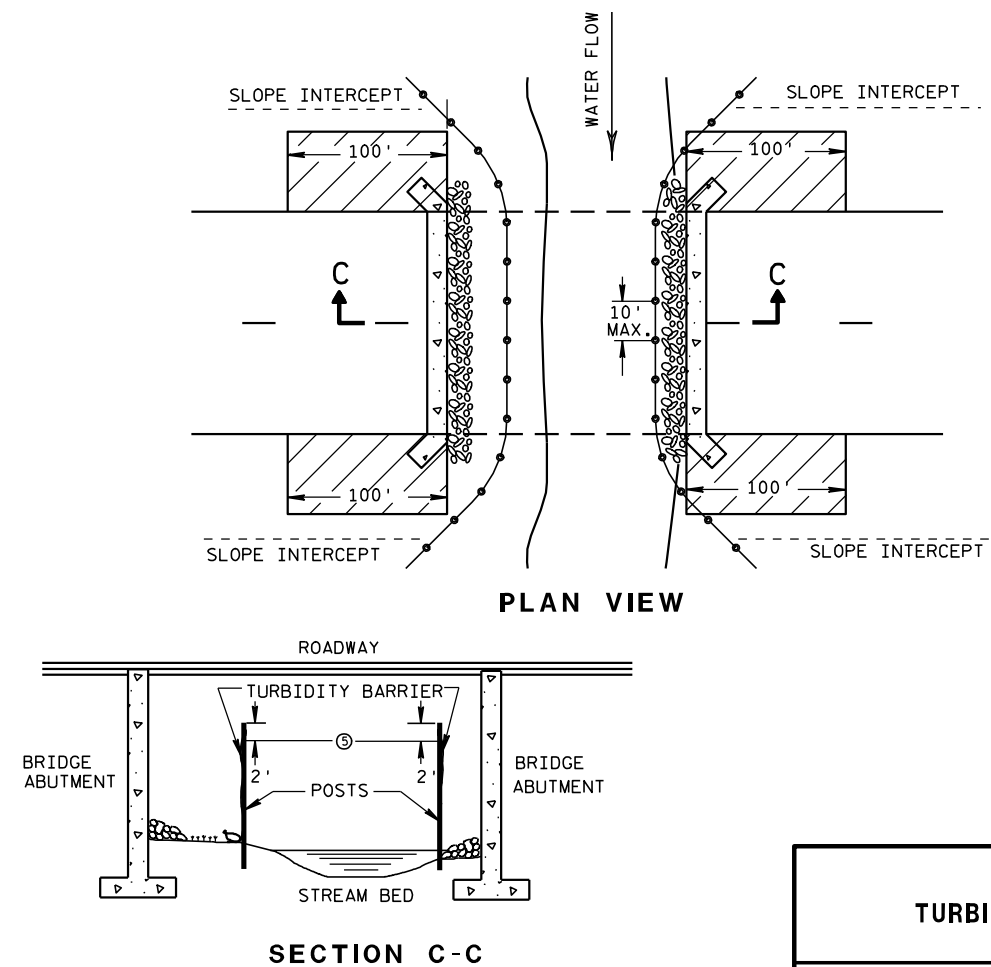


GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- ② SANDBAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- ③ WHEN BARRIER HEIGHT, H, EXCEEDS 8 FT., POST SPACING MAY NEED TO BE DECREASED.
- ④ IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- ⑤ ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE 02 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
- ⑥ FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BED ROCK PREVENTS THE INSTALLATION OF POSTS.
- ⑦ ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- ⑧ USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

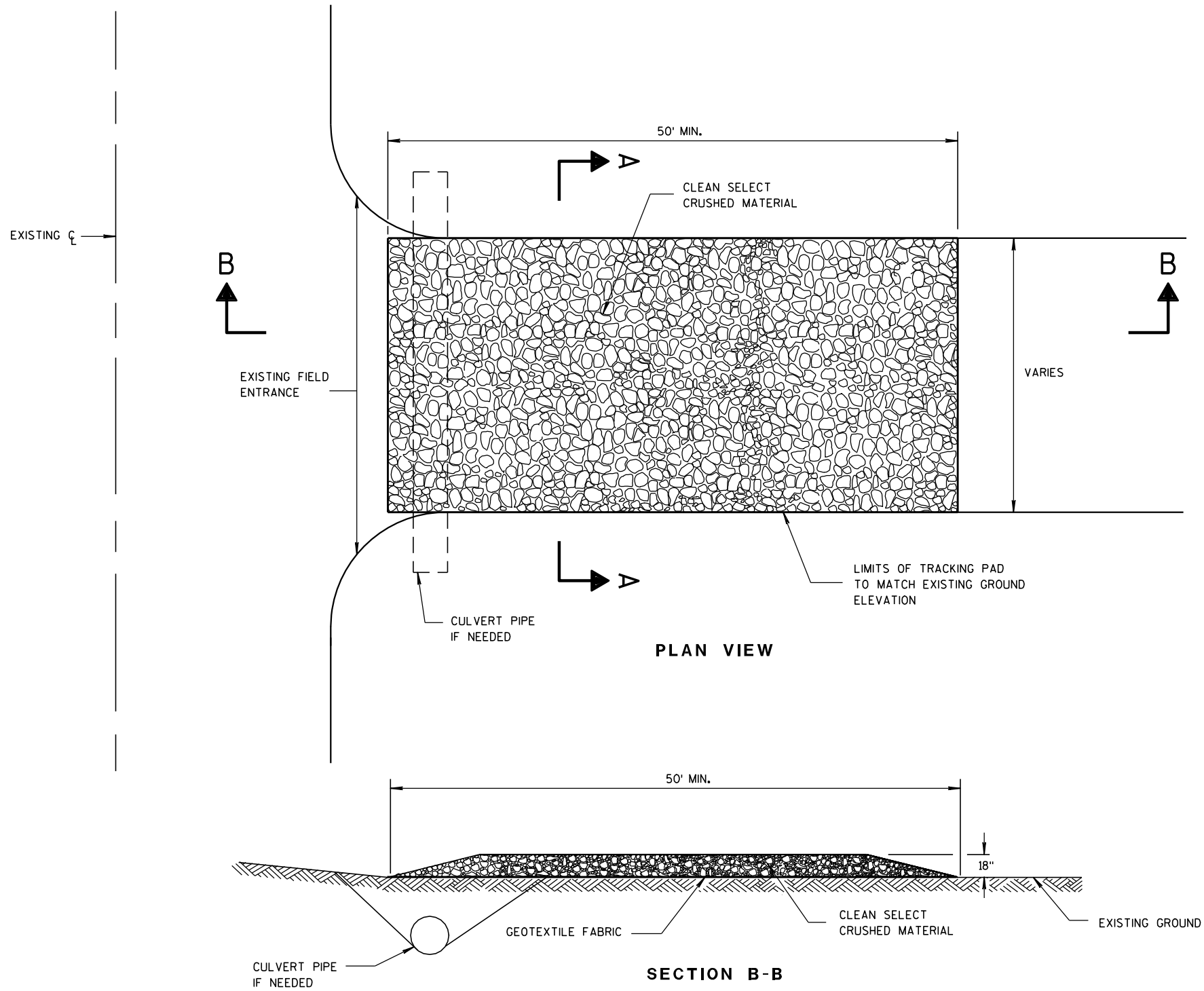
APPROVED

6/04/02
DATE

FHWA

/S/ Beth Connestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES



TRACKING PAD

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TRACKING PAD SHALL BE INSPECTED DAILY. DEFICIENT AREAS SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

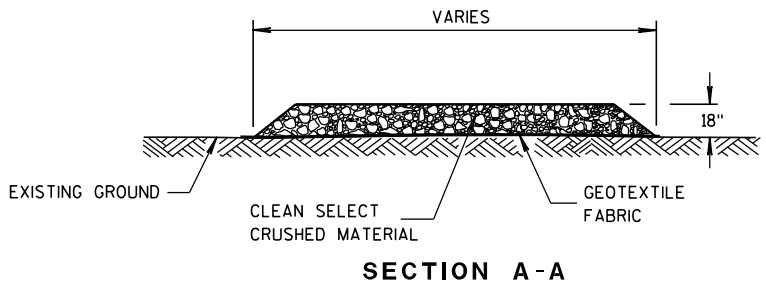
TRACKING PAD TO BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT.

SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY, AROUND OR CONVEYED UNDER THE TRACKING PAD.

CULVERT PIPE OR OTHER BMP USED TO DIVERT WATER AWAY, AROUND OR UNDER THE TRACKING PAD SHALL BE DESIGNED TO CONVEY THE 2 YEAR - 24 HOUR EVENT.

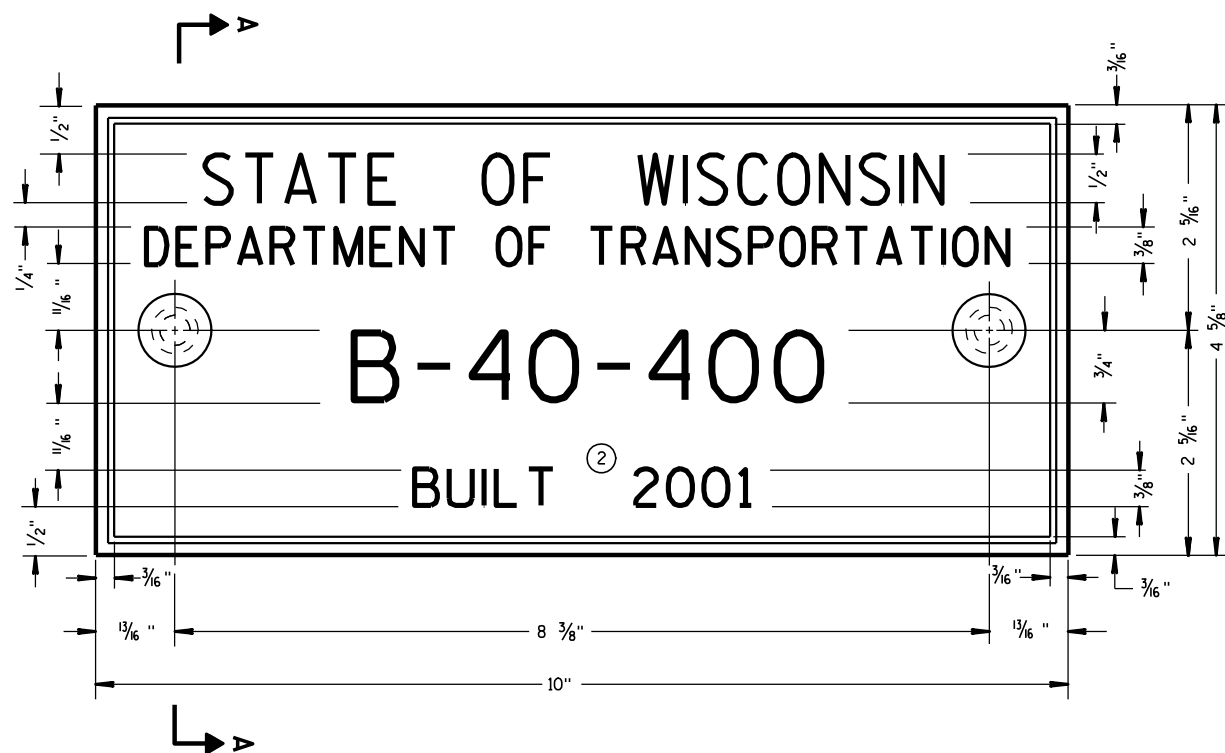
THE COST OF ADDITIONAL BMP TO DIVERT WATER ARE INCIDENTAL TO THE TRACKING PAD BID ITEM.



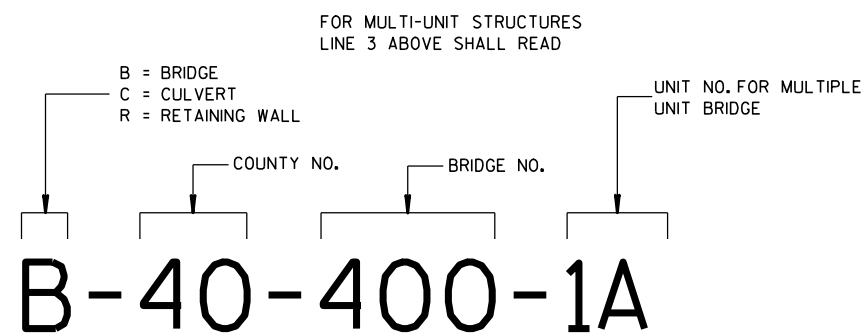
TRACKING PAD

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
3/24/2011
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)



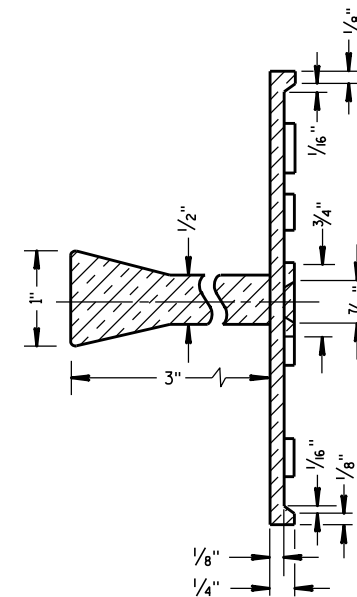
**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

GENERAL NOTES

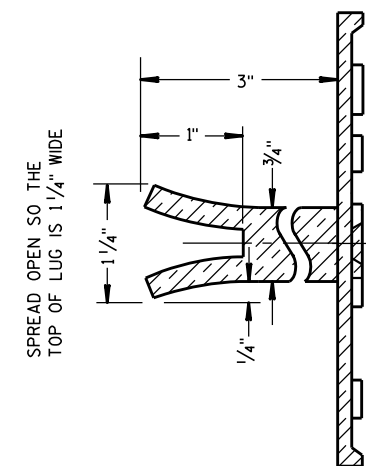
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

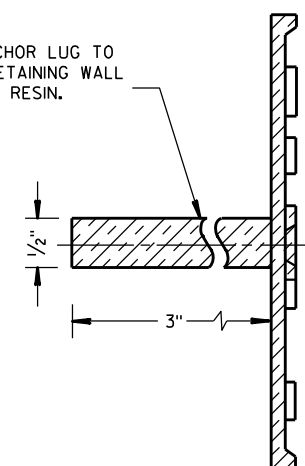


SECTION A-A



ALTERNATE LUG

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

**NAME PLATE
(STRUCTURES)**

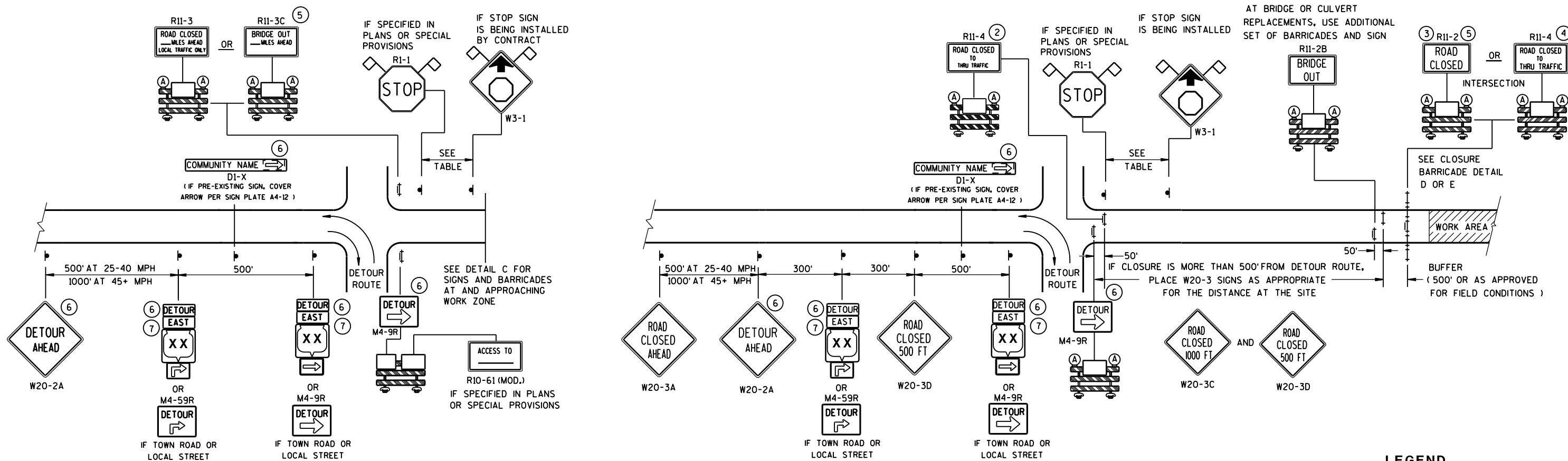
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/26/10
DATE

FHWA

/S/ Scot Becker
CHIEF STRUCTURAL DEVELOPMENT ENGINEER



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)

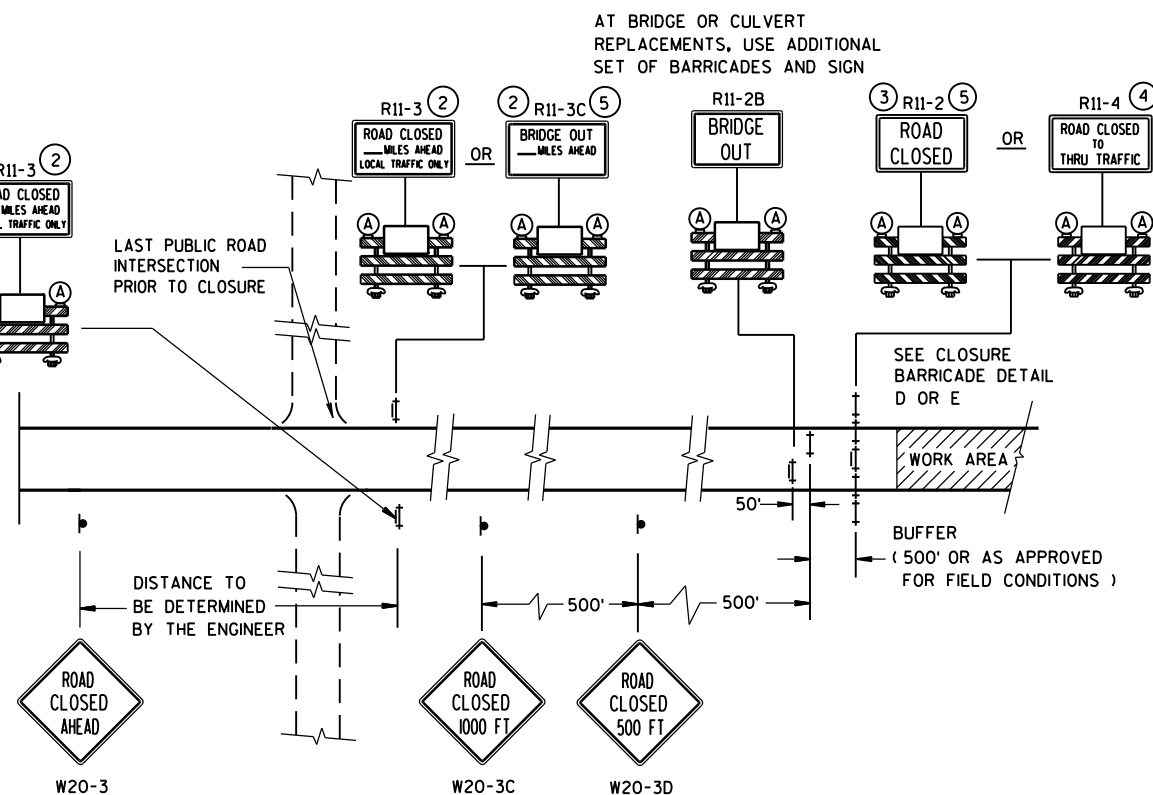
WORK AREA

DETOUR EAST M4-8
M3-X
XX OR COUNTY XX OR XX
M1-4 M1-5A M1-6

M05-1 OR M06-1

FLAGS, 16" X 16" MIN., (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750



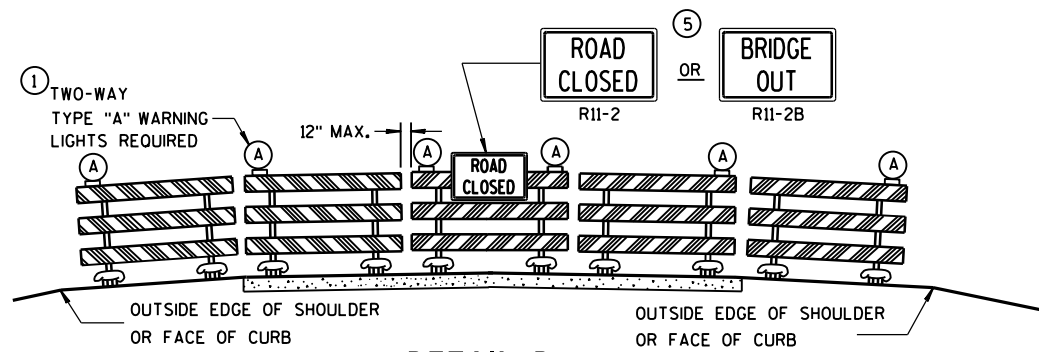
DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

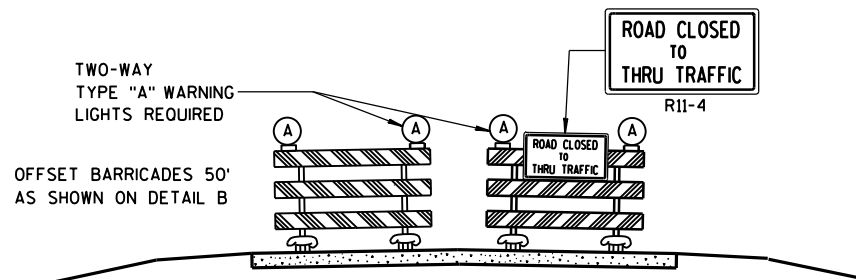
**BARRICADES AND SIGNS
FOR
MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 /S/ Peter Amokobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

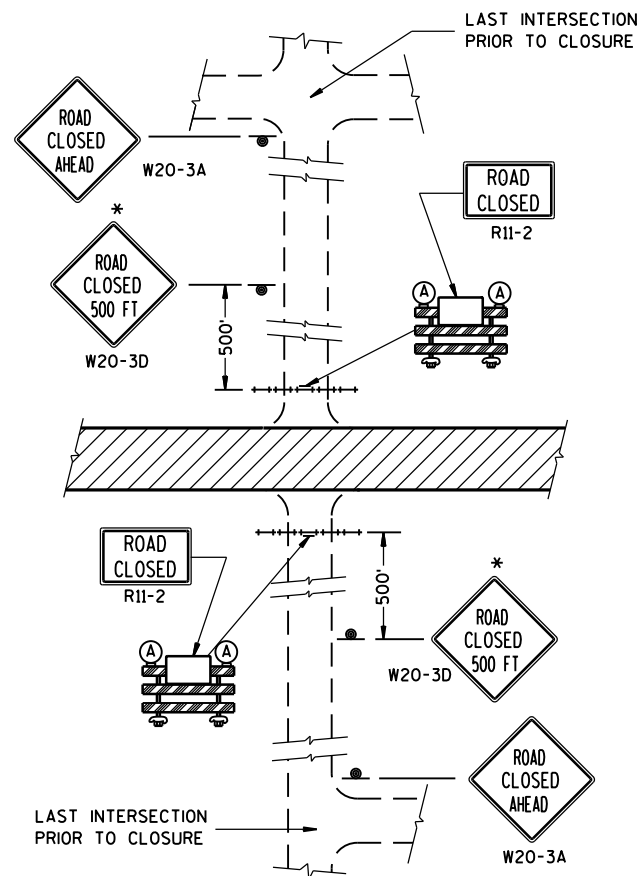
"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

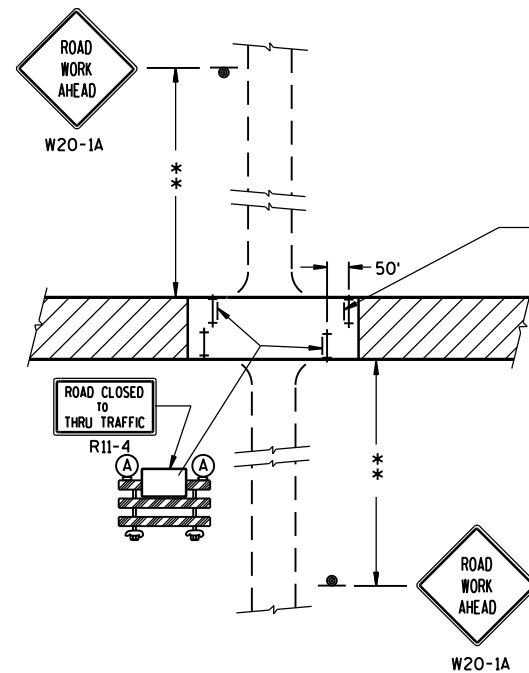
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

- 1 TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- 2 THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- 3 FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- 4 FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- 5 FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- 6 INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- 7 "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

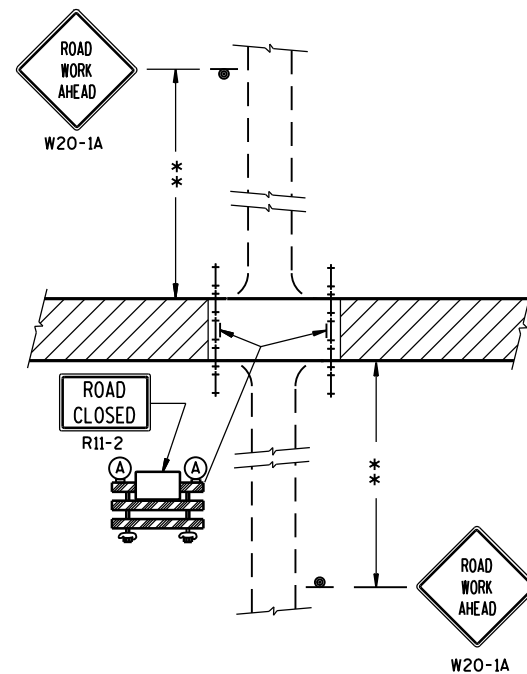
BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Sept. 2015 DATE	/S/ Peter Amokobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

**DETAIL 1**

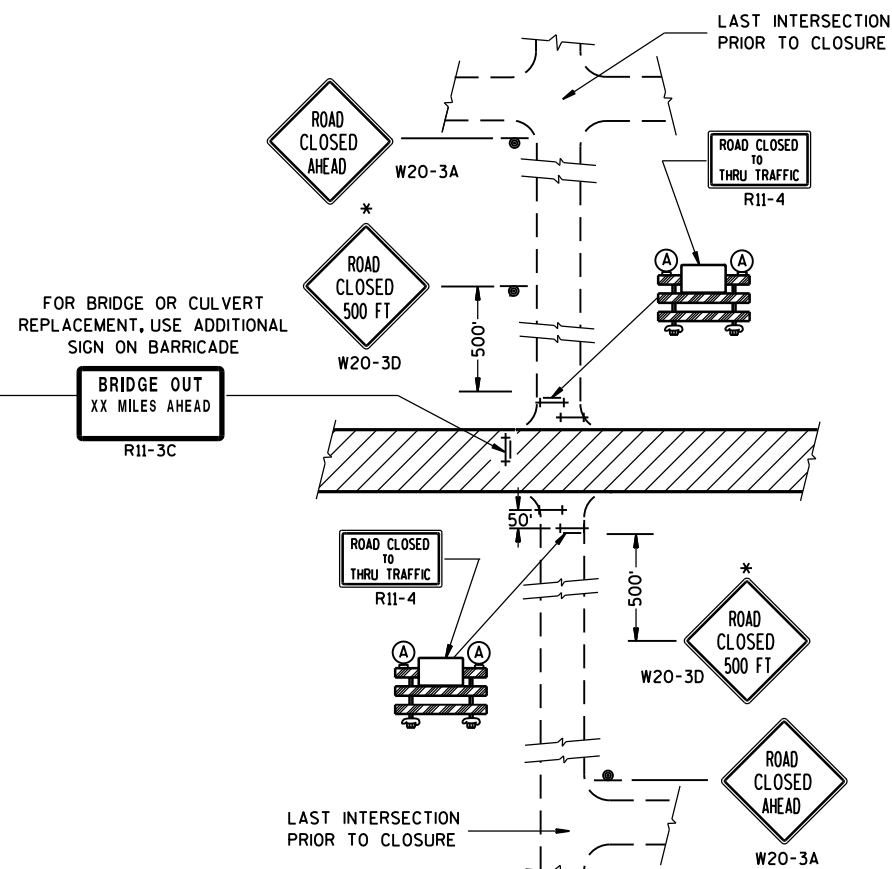
(NO ACCESS TO PROJECT)

**DETAIL 3**

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).

**DETAIL 2**

(PUBLIC CROSS-TRAFFIC MAINTAINED. NO ACCESS TO PROJECT).

**DETAIL 4**

(CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

Sept. 2015

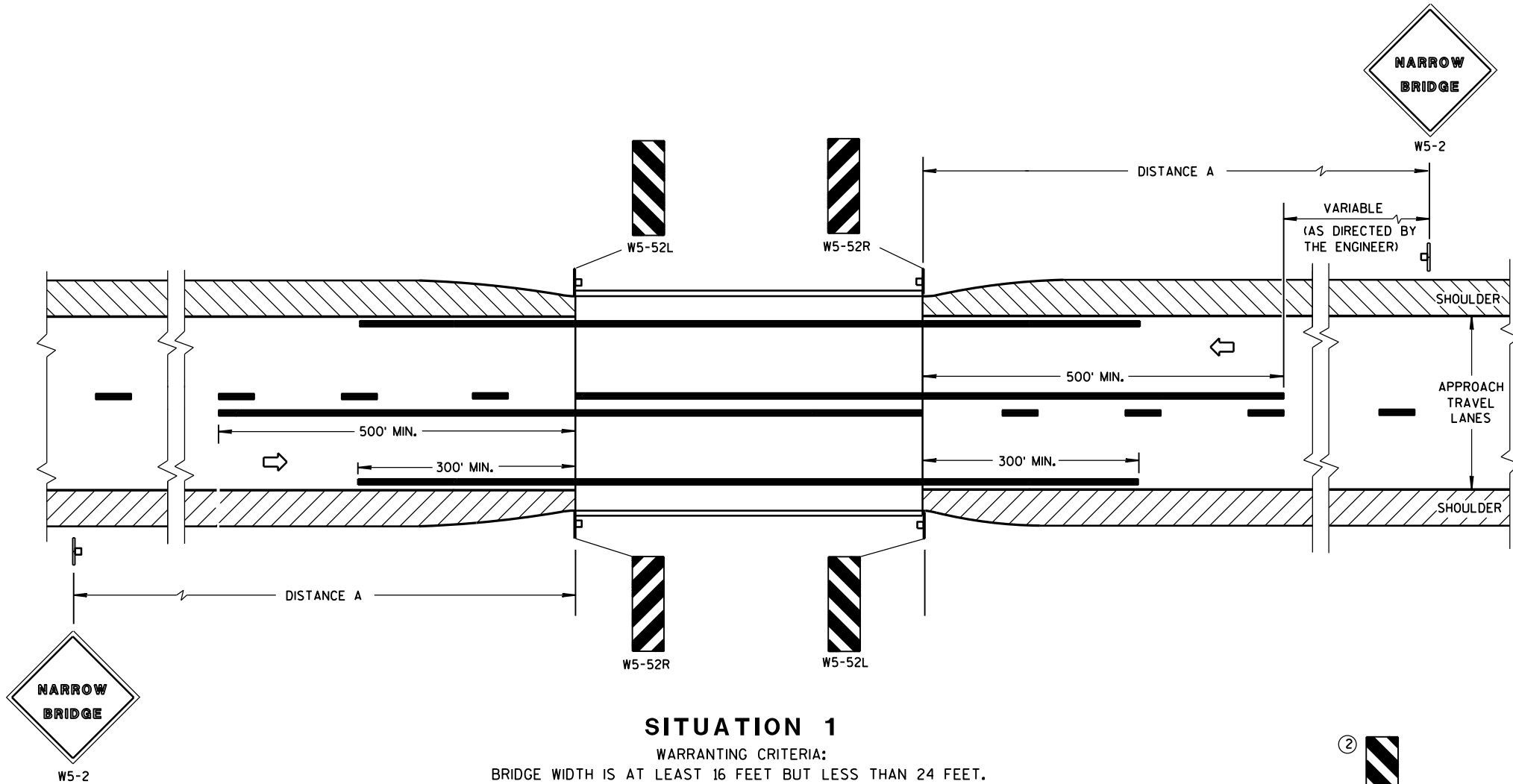
DATE

FHWA

/S/ Peter Amakobe Atepe

STATEWIDE WORK ZONE TRAFFIC

SAFETY ENGINEER



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.

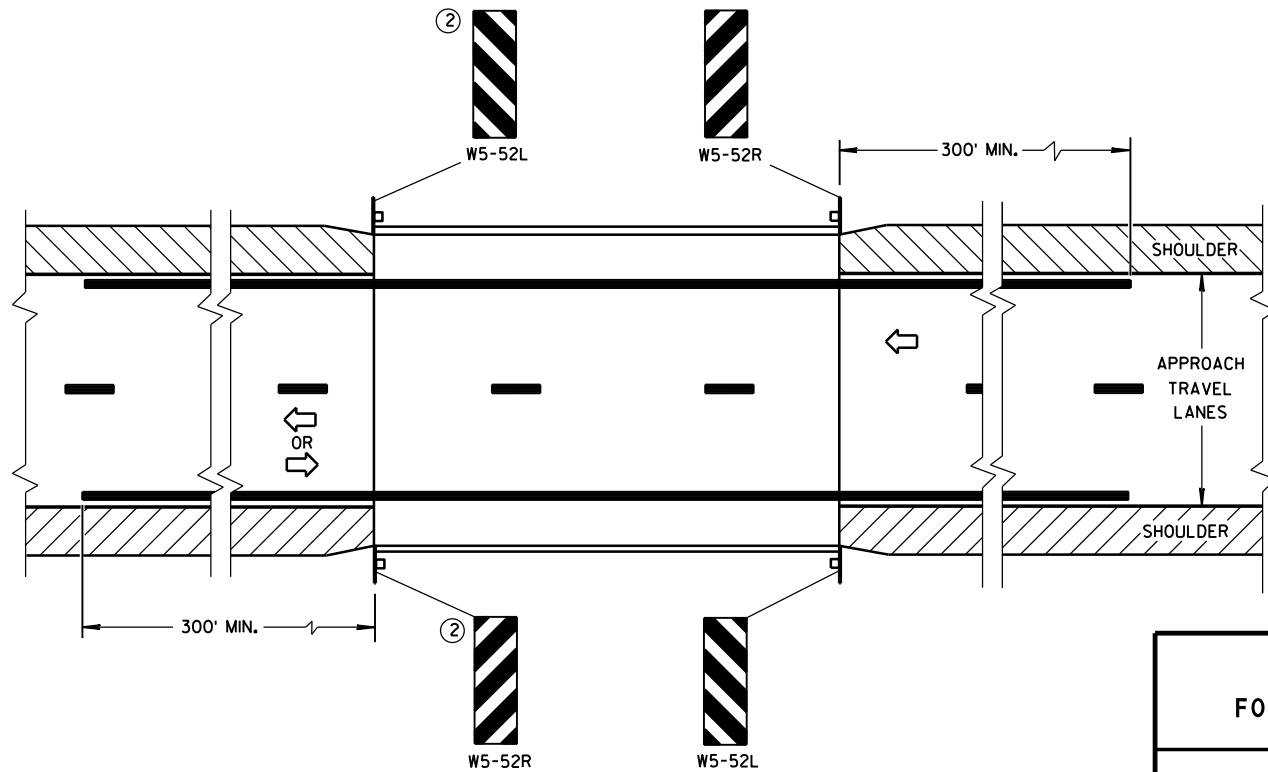
DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

- ① LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.
- ② OMIT ON ONE-WAY TRAVELLED WAYS.
- ③ EDGE OF W5-52 SIGN SHALL BE PLACED IN LINE WITH FACE OF CURB OR PARAPET.



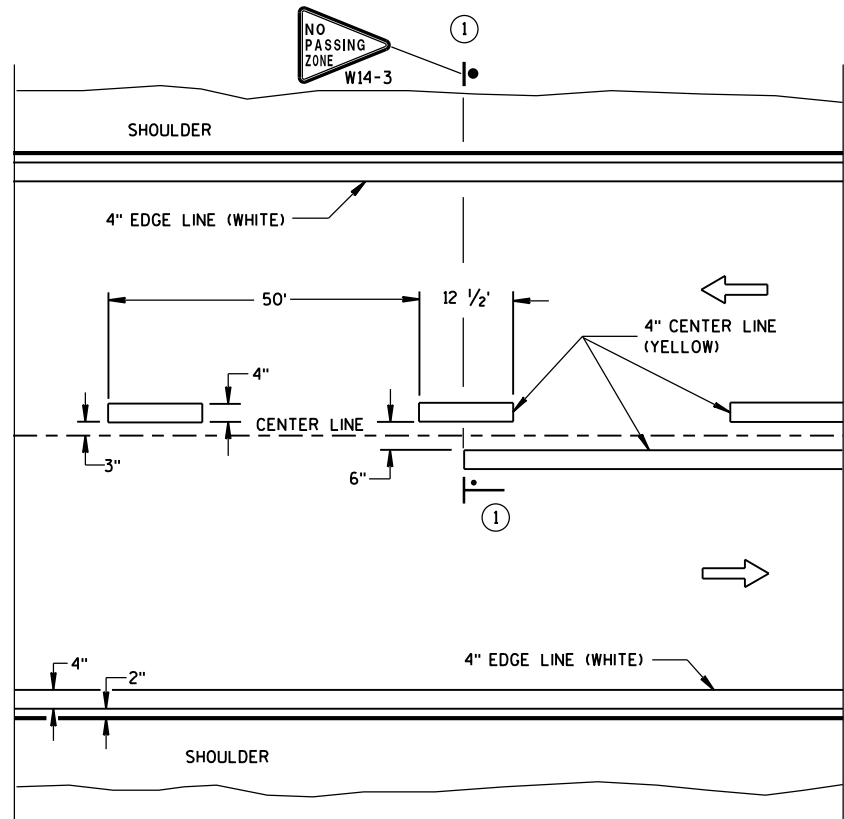
SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET.

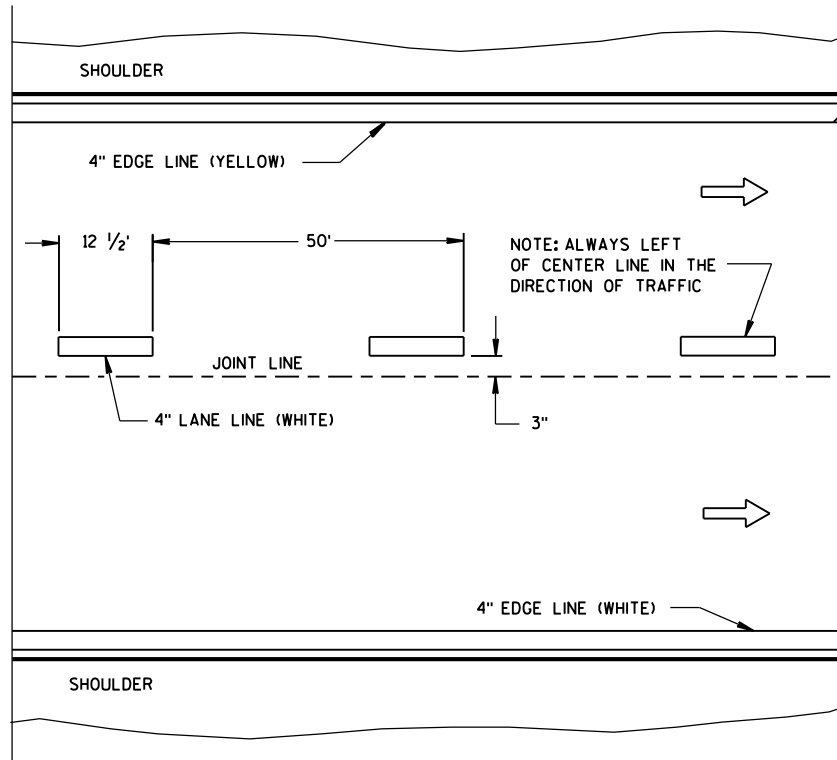
SIGNING & MARKING FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-18-16 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

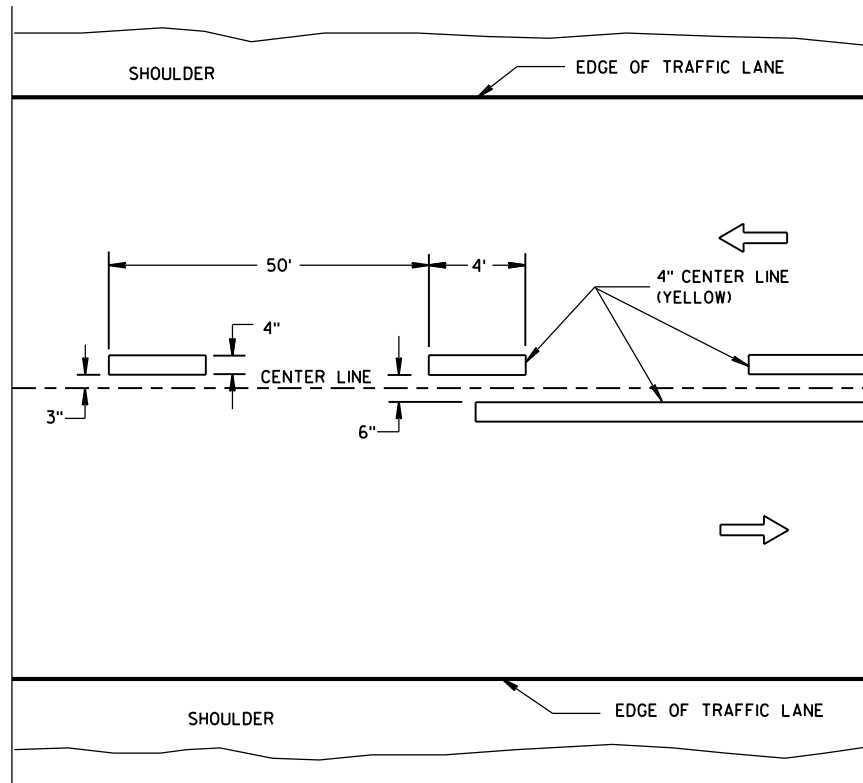


TWO WAY TRAFFIC

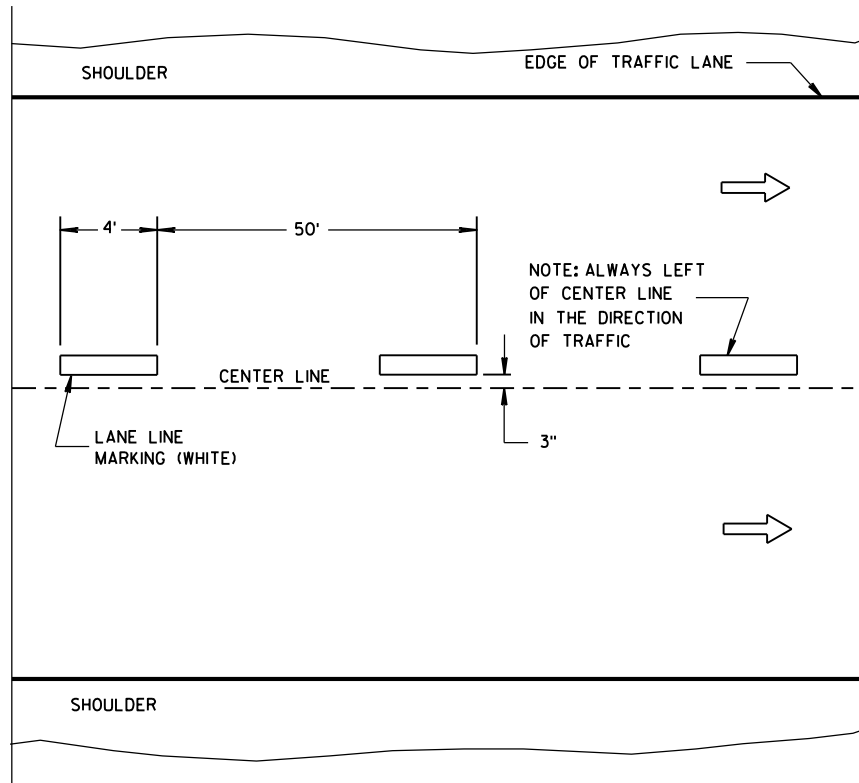


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

① NO PASSING ZONE W14-3 SIGN SHALL BE LOCATED WITHIN 50 FEET OF THE "T" MARKING.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

LEGEND

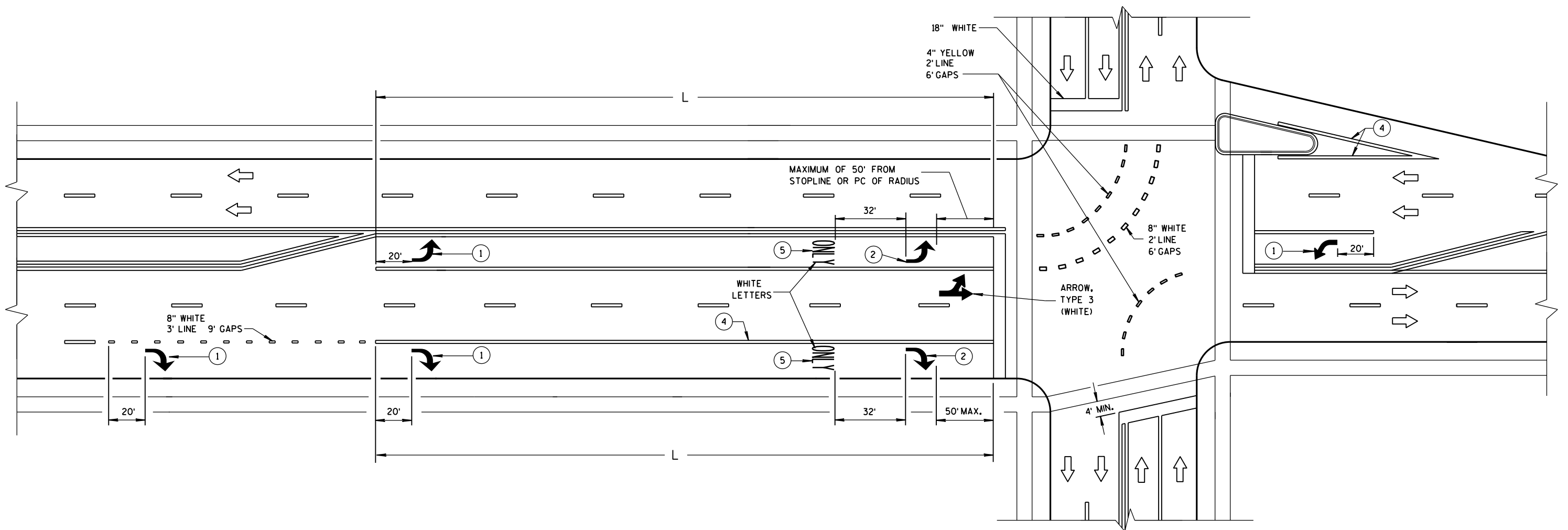
—●— "T" MARKING

● POST MOUNTED SIGN

LONGITUDINAL MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION


APPROVED
Sept., 2016 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA



GENERAL NOTES

- ① REQUIRED ARROW, TYPE 2 (WHITE).
- ② REQUIRED ARROW, TYPE 2 (WHITE) WHEN L IS GREATER THAN 78 FEET AND LESS THAN OR EQUAL TO 166 FEET.
- ③ A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ④ 8" WHITE
- ⑤ REQUIRED WORD ONLY WHEN L IS GREATER THAN 166 FEET.

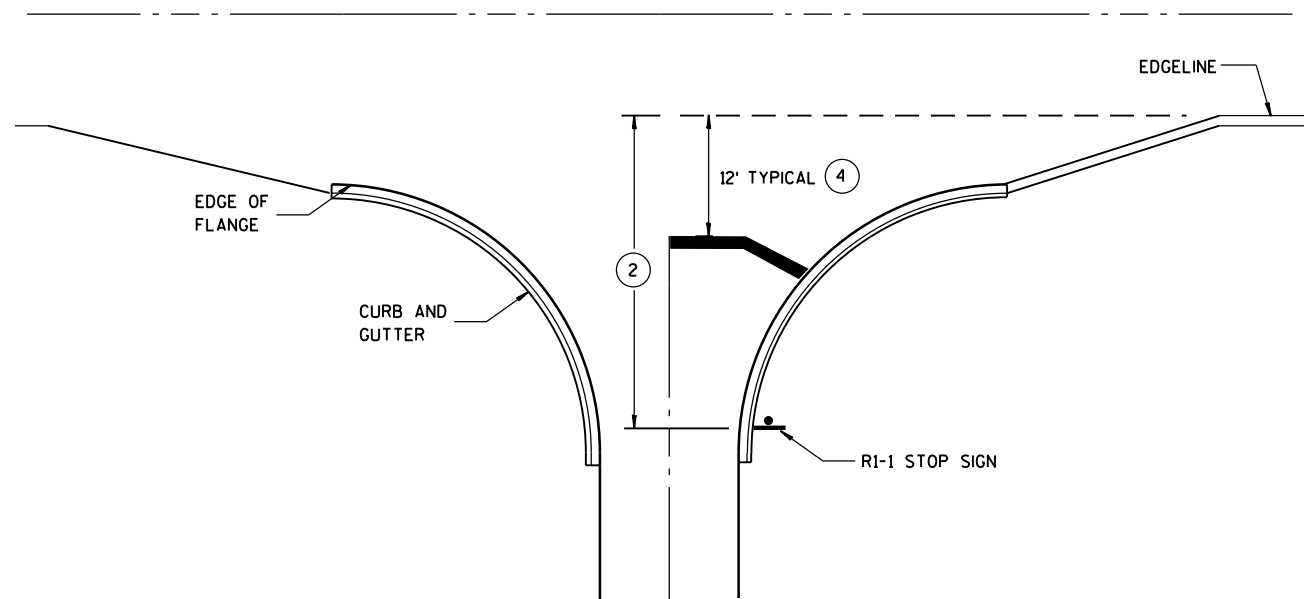
TWO WAY LEFT TURN LANE

NOTE:
ARROW SYMBOL ()
SHOWS DIRECTION OF TRAVEL

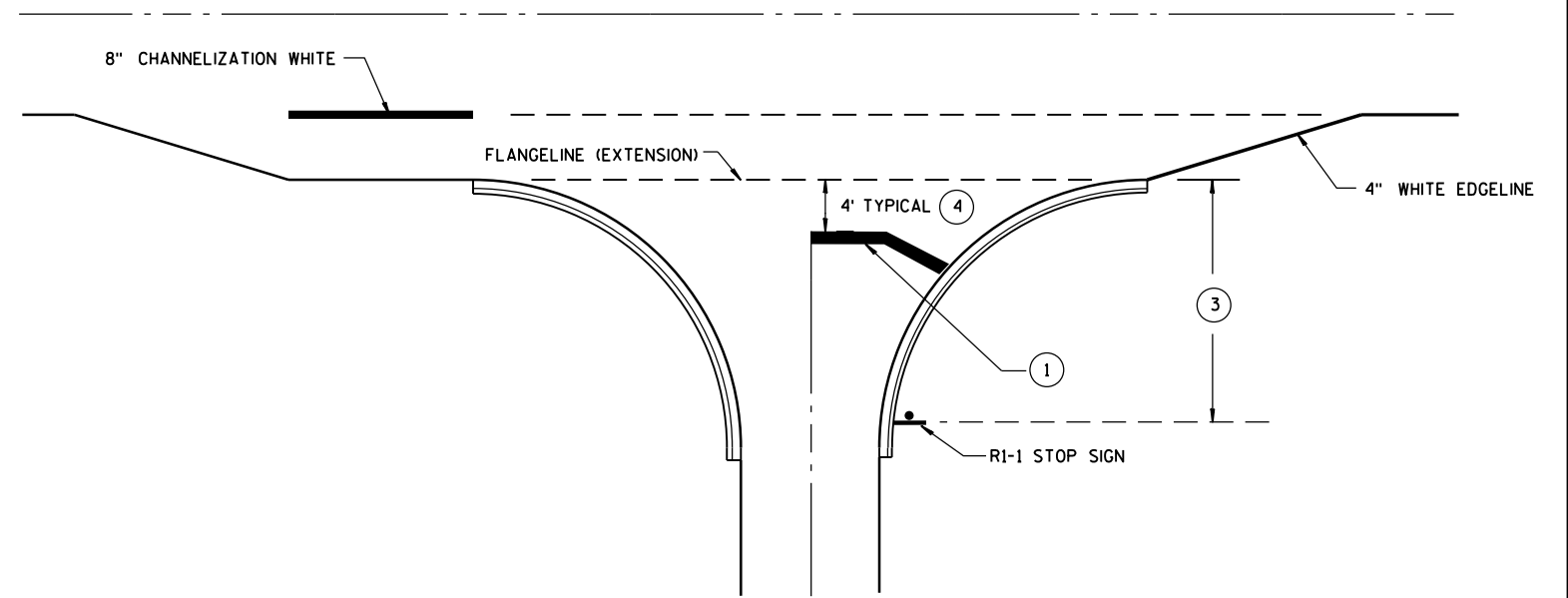
L = LENGTH OF TURN BAY

PAVEMENT MARKING
(TURN LANES)

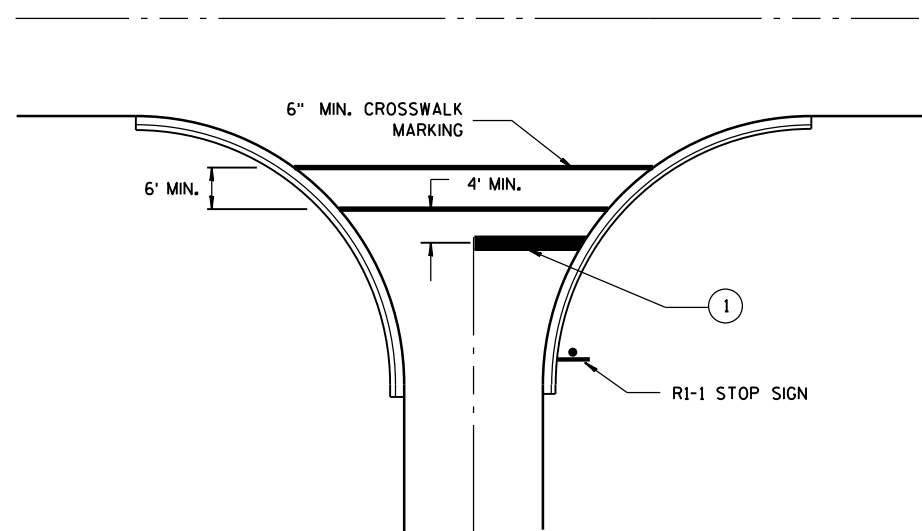
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



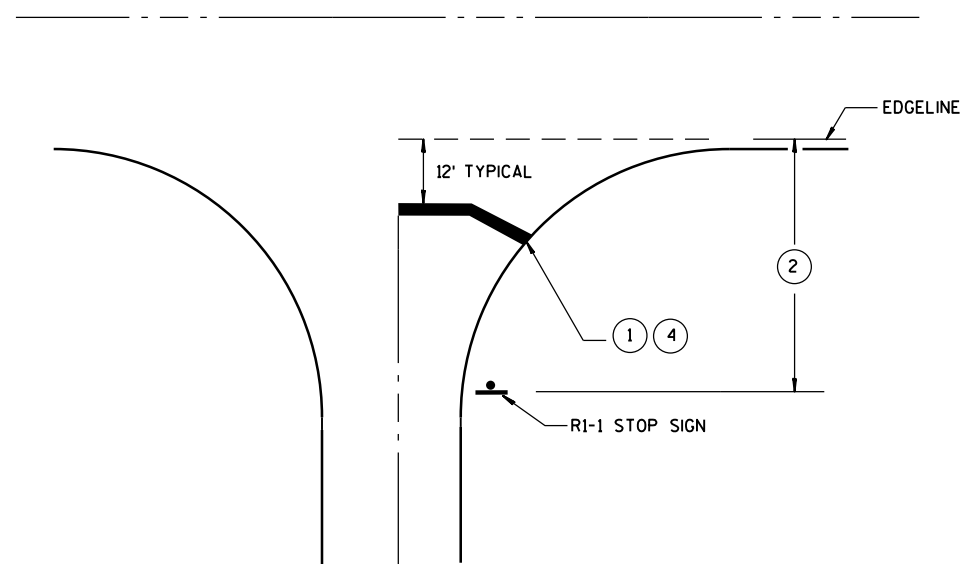
**TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER**



**TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE**



**TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH CROSSWALK MARKING**



**TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER**

GENERAL NOTES

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

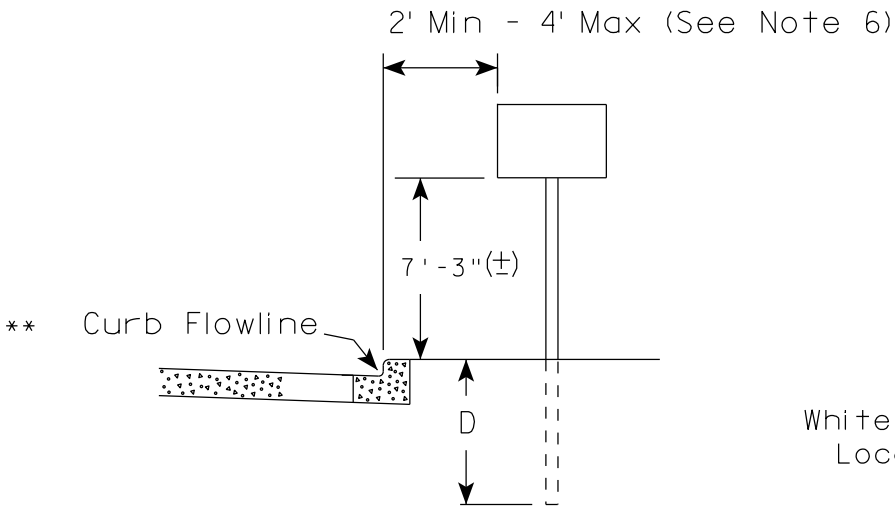
APPROVED

4-18-2016
DATE

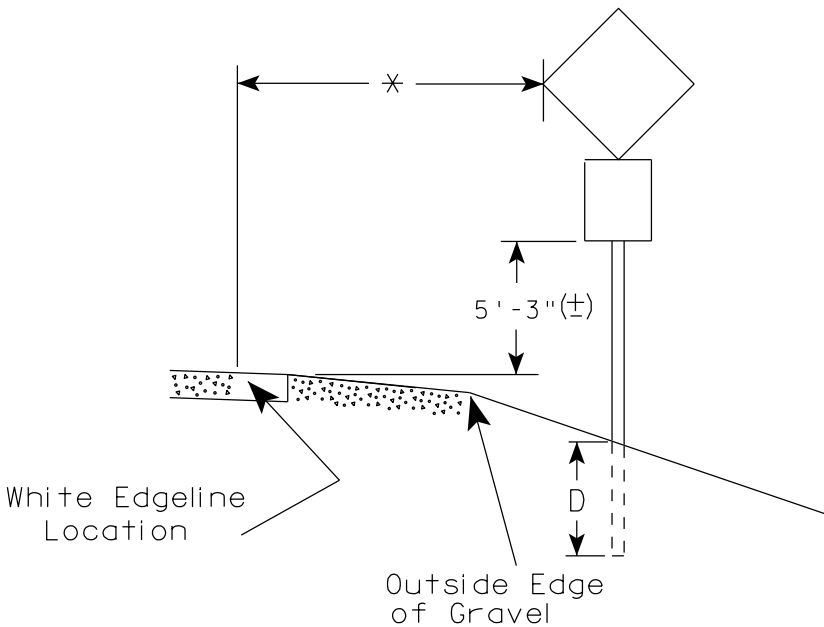
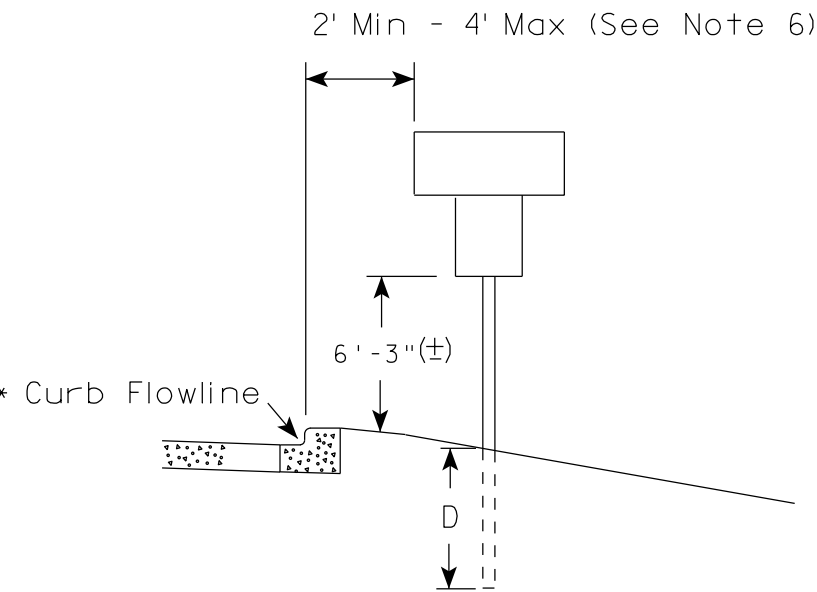
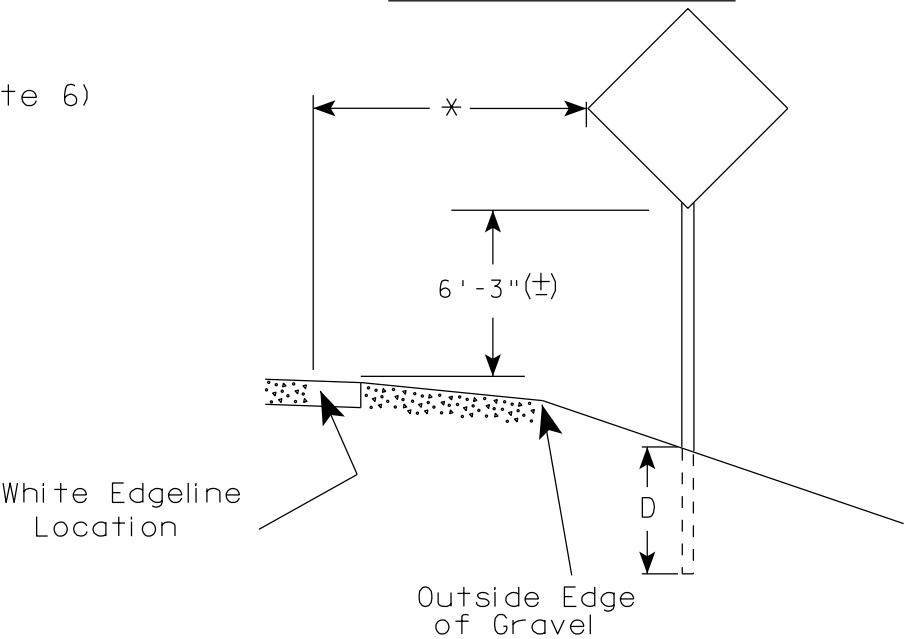
FHWA

/S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

×× The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

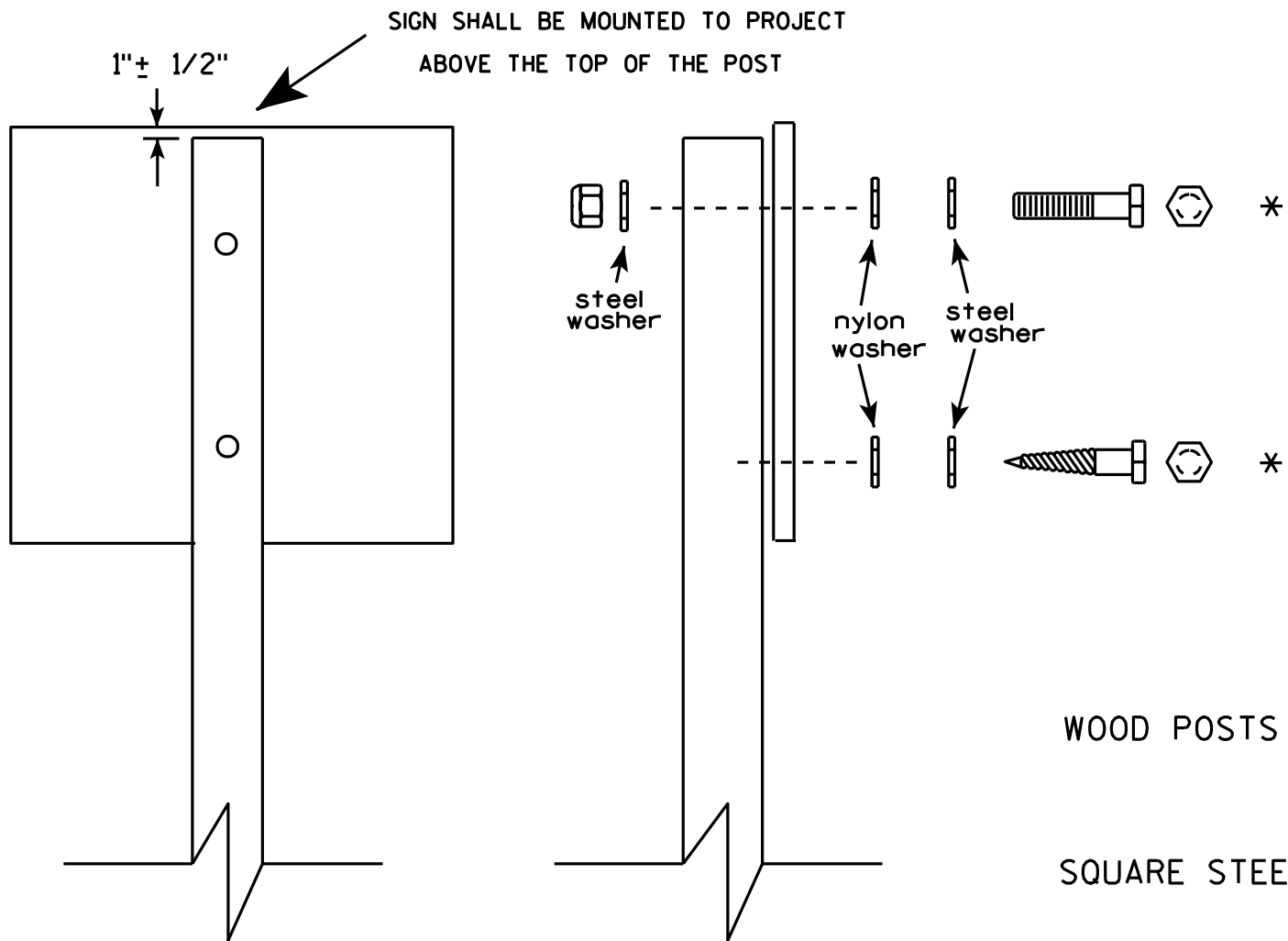
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-3.20

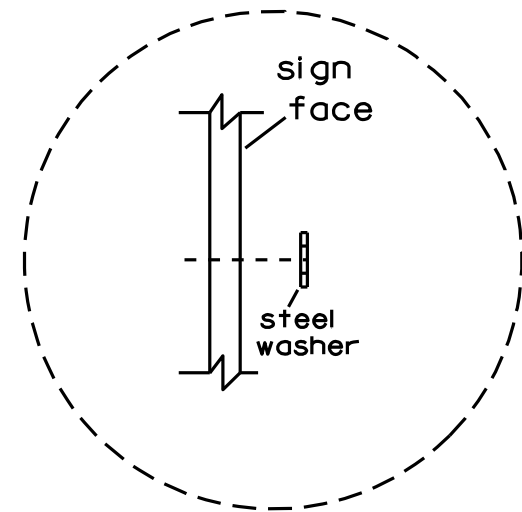


Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

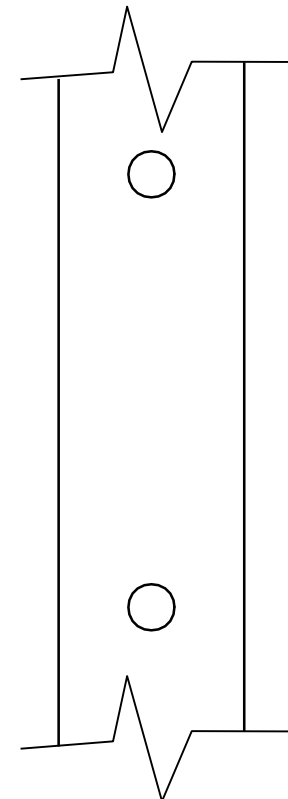
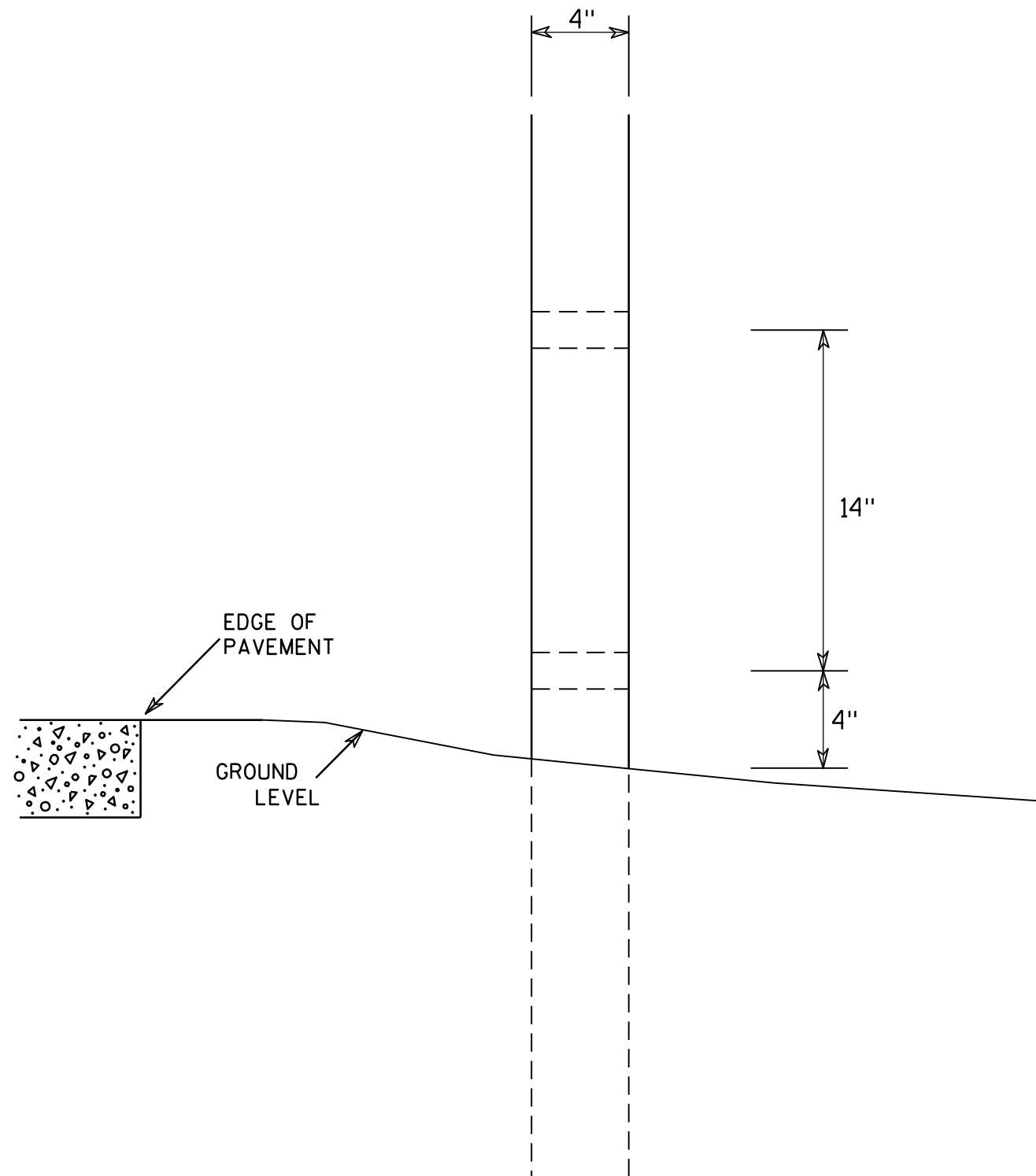
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3"
- MACHINE BOLTS - $\frac{5}{16}$ " X 6-1/2" or 7" Length w/ nuts
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON for all Type H signs.



Washer Placement when Sign Has Other Than Type H or Type F Face

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/23/10	PLATE NO. A4-8.7



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

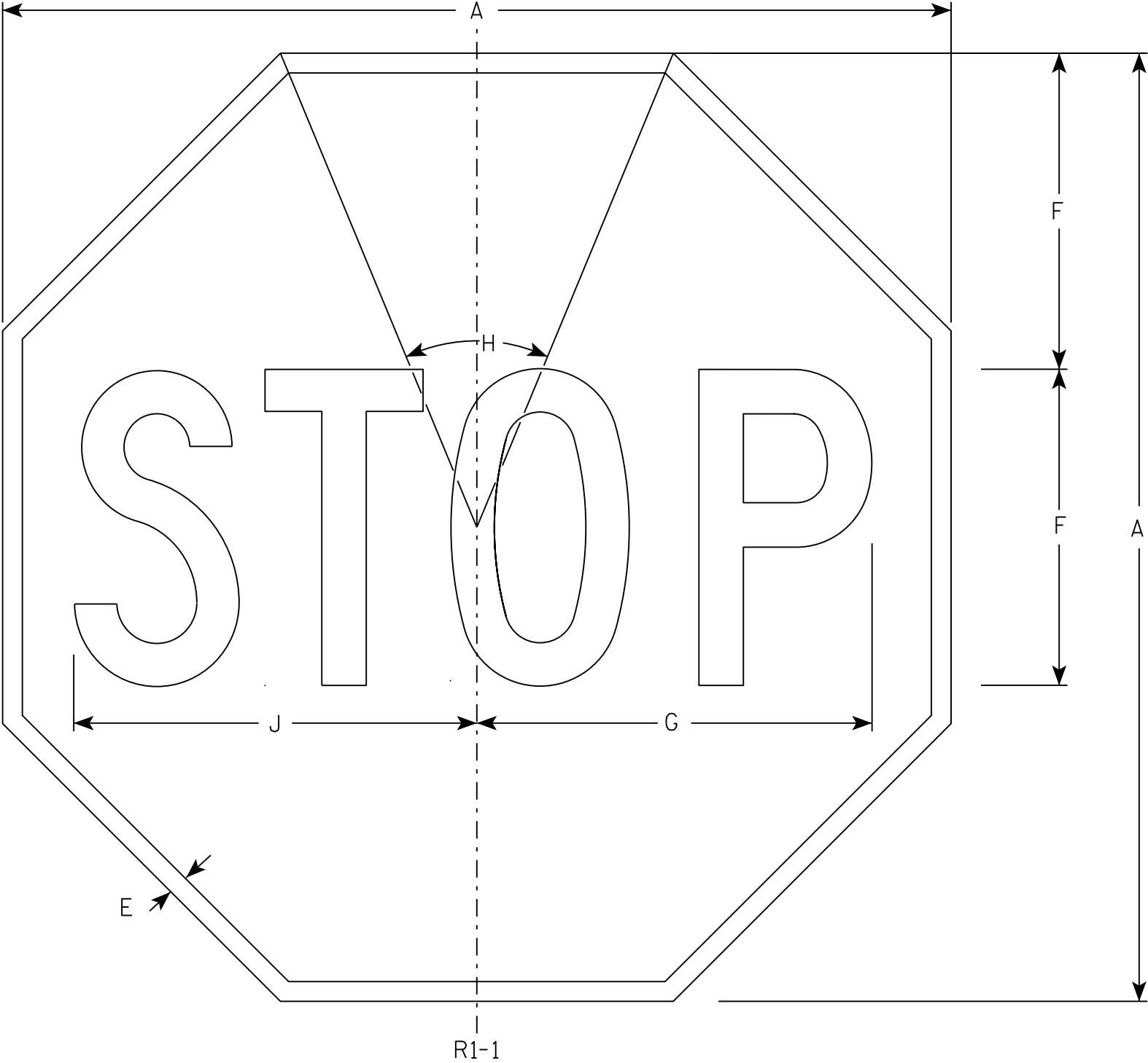
HWY:

COUNTY:

SHEET NO:

E

7



NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Red
Message - White
- 3. Message Series - C

7

R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN
R1 - 1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

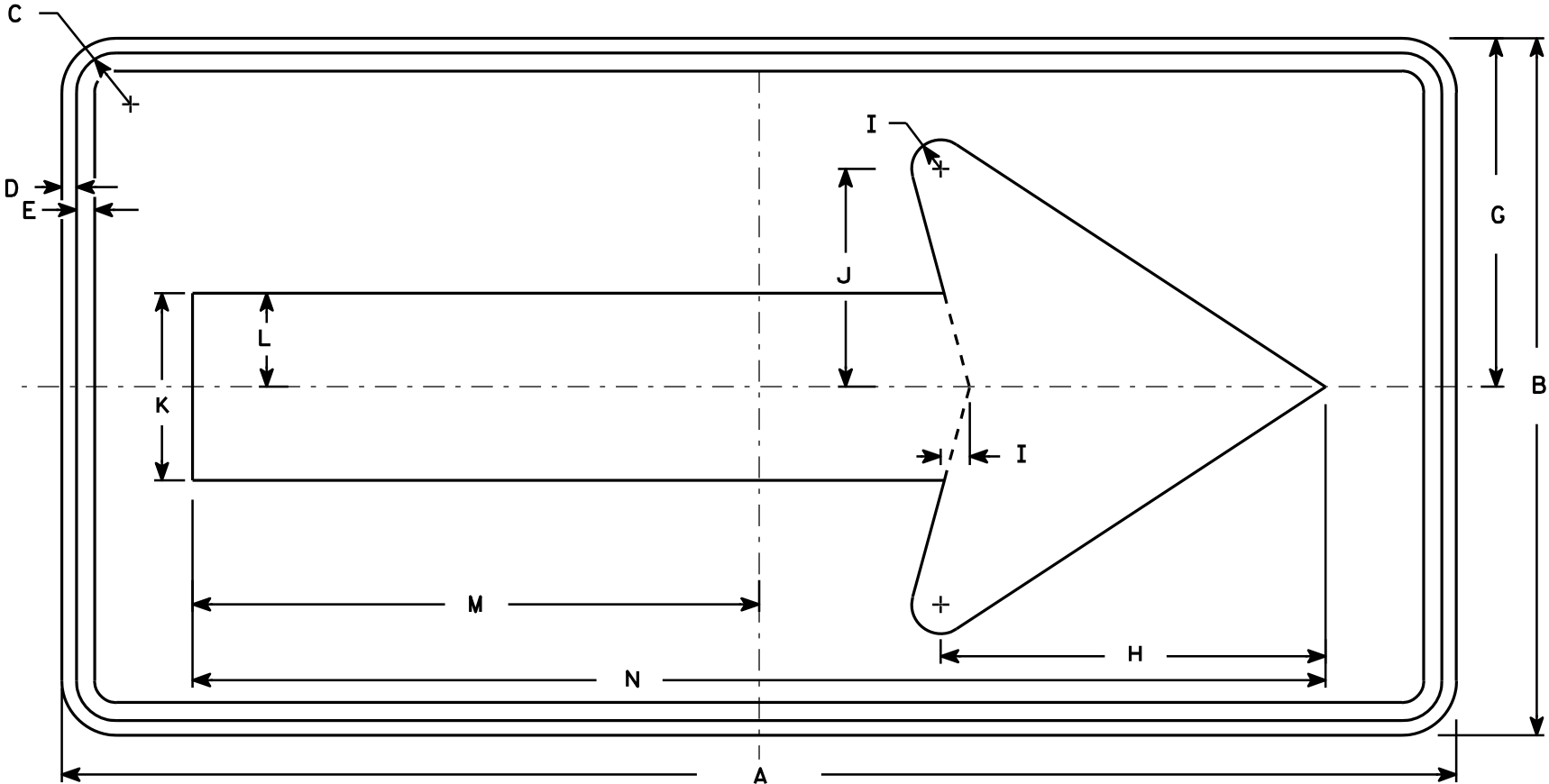
DATE 11/12/15 PLATE NO. R1-1.13

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:

Background - Yellow

Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



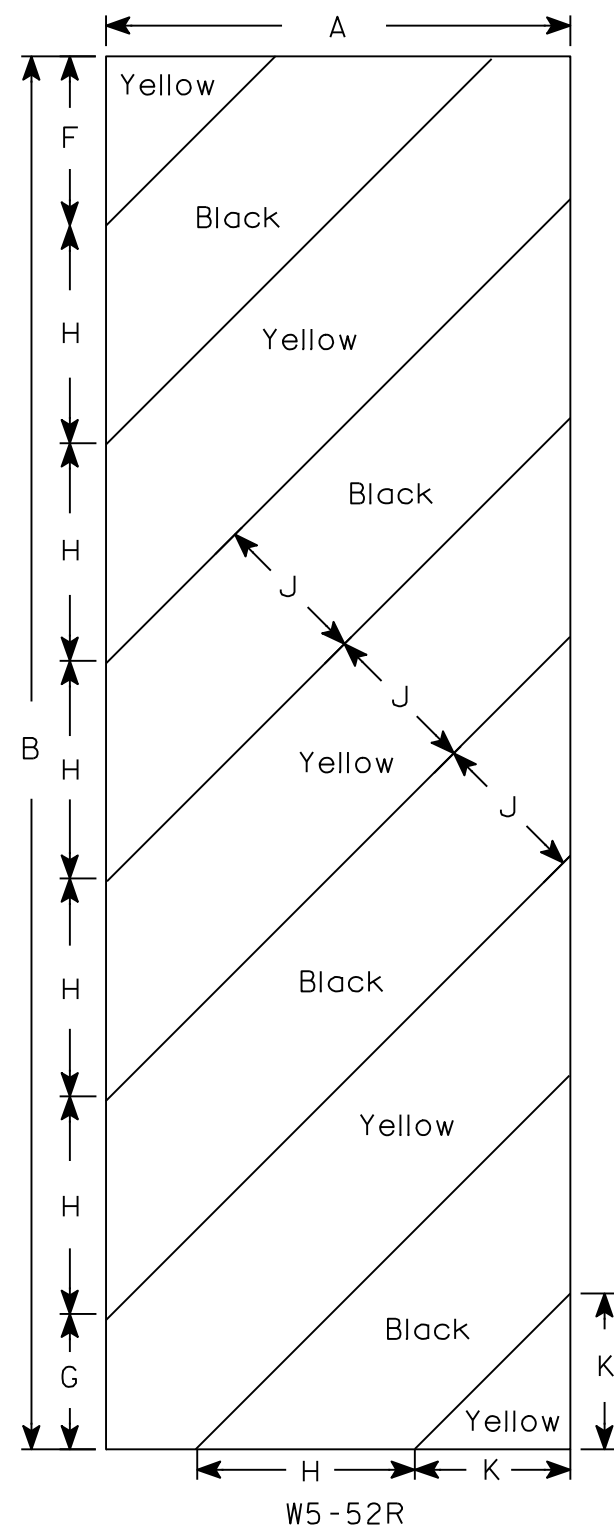
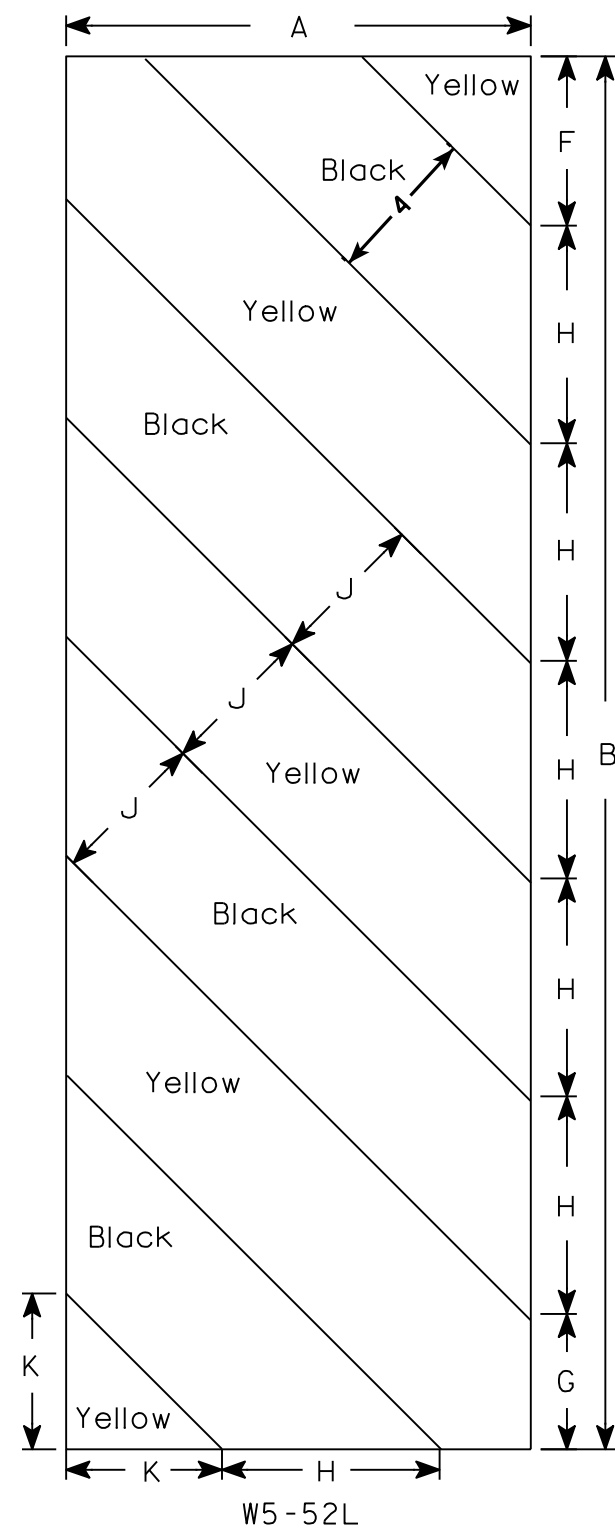
W1-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	3/8		9	10	3/4	5 5/8	4 3/4	2 3/8	14 5/8	29 1/4													4.5
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	96	48	2 1/4	3/4	1		24	26 1/2	2	15	13	6 1/2	39	78													32.0

STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/7/10 PLATE NO. W1-6.8



NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3⁄8	3 1⁄2	5 5⁄8	45°	4	4																3.0
2M	12	36				4 3⁄8	3 1⁄2	5 5⁄8	45°	4	4																3.0
3	18	54				6	5 1⁄2	8 1⁄2	45°	6	6 9⁄16																6.75
4																											
5																											

STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

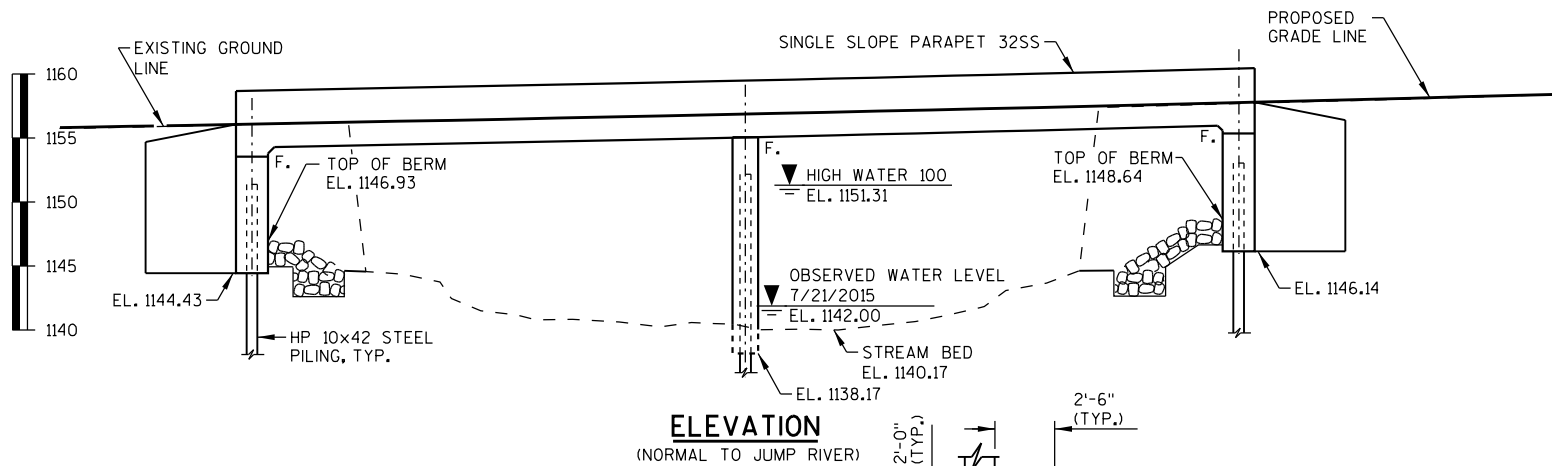
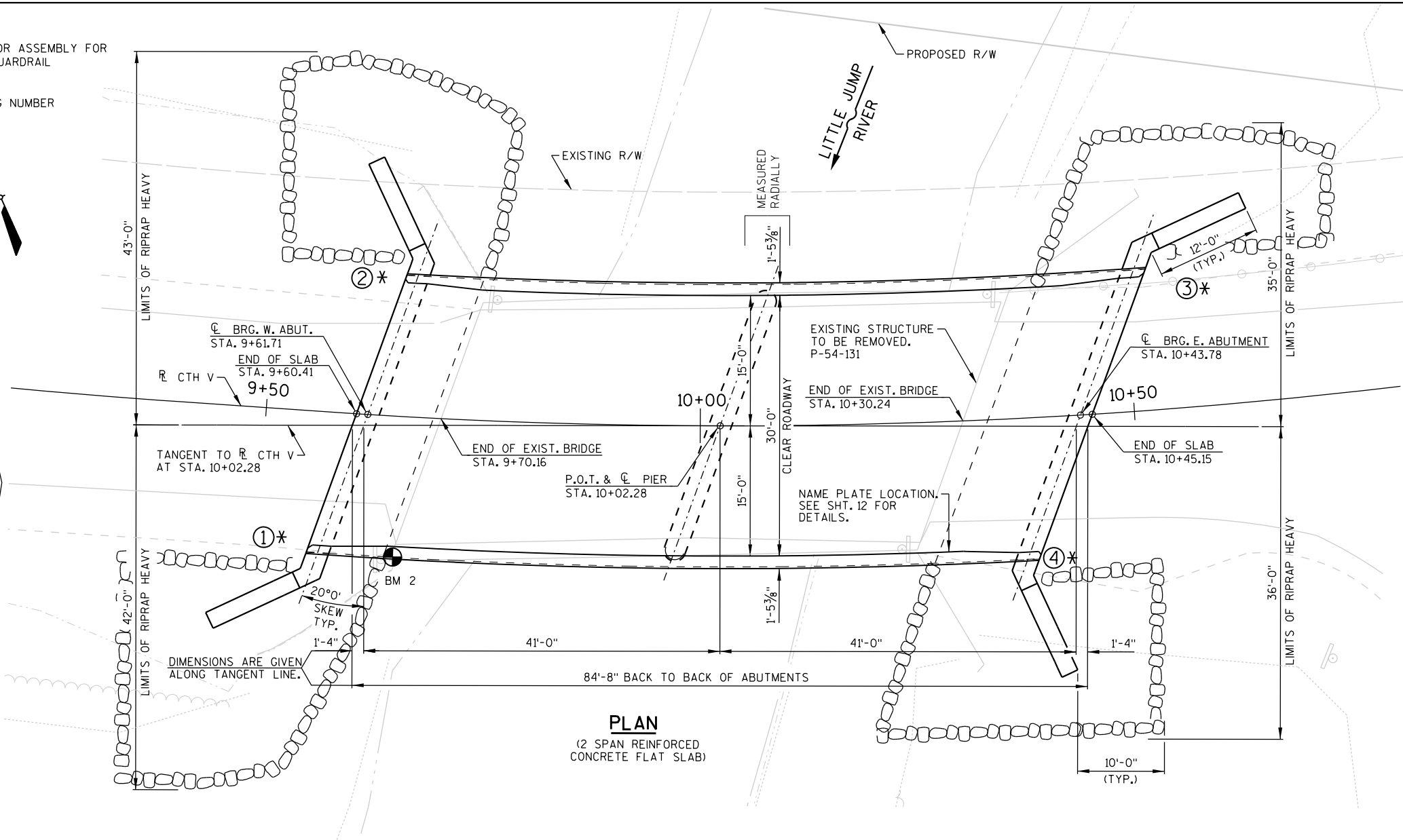
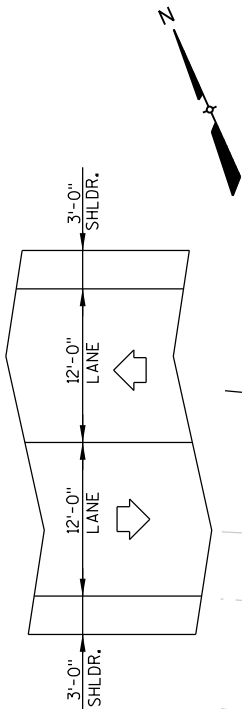
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PLOT TIME: 3:58:22 PM

BATCH PRINT SHEET 1 OF 1

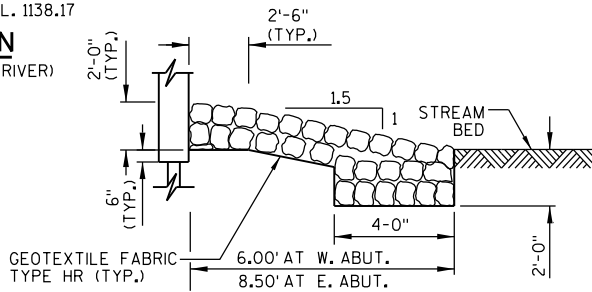
8

- * PROVIDE ANCHOR ASSEMBLY FOR THRE BEAM GUARDRAIL ATTACHMENT.
① INDICATES WING NUMBER



CURVE DATA: CTH V

P.L. = 11+22.31
Y = 526897.05
X = 848312.19
DELTA = 27° 55' 07"
D = 8° 40' 52"
T = 164.06'
L = 321.60'
R = 660.00'
P.C. = 9+58.25
P.T. = 12+79.85



RIPRAP DETAIL

DESIGN DATA

LIVE LOAD:
DESIGN LOADING: HL-93
INVENTORY RATING FACTOR = 1.37
OPERATING RATING FACTOR = 1.76
MAXIMUM STANDARD PERMIT VEHICLE LOAD = 250 KIPS

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 PSF.

ULTIMATE DESIGN STRESSES:

CONCRETE MASONRY -
SLAB $f'_c = 4,000$ P.S.I.
ALL OTHER $f'_c = 3,500$ P.S.I.
BAR STEEL REINFORCEMENT, GRADE 60 $f_y = 60,000$ P.S.I.

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON HP10x42 STEEL PILING, PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 180 TONS** PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 30'-0" LONG FOR THE WEST ABUTMENT AND 30'-0" LONG FOR THE EAST ABUTMENT.

PIER TO BE SUPPORTED ON HP10x42 STEEL PILING, PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 180 TONS** PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 30'-0" LONG.

** THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING THE MODIFIED GATE DYNAMIC FORMULA TO DETERMINE DRIVEN PILE CAPACITY.

TRAFFIC VOLUME

CTH V
A.D.T. (2018) = 340
A.D.T. (2038) = 375
DESIGN SPEED = 45 MPH

HYDRAULIC DATA

100 YEAR FREQUENCY
Q100 = 4,270 CFS
VELOCITY = 8.7 FPS
HIGH WATER ELEVATION = 1151.31
WATERWAY AREA = 491 SQ. FT.
DRAINAGE AREA = 47.7 SQ. MI.
ROAD OVERTOPPING = N/A
SCOUR CRITICAL CODE = PENDING

2 YEAR FREQUENCY
Q2 = 1,220 CFS
HIGH WATER 2 ELEVATION = 1147.15

LIST OF DRAWINGS

- GENERAL PLAN
- CROSS SECTION & QUANTITIES
- SUBSURFACE EXPLORATION
- WEST ABUTMENT
- WINGS 1 & 2
- EAST ABUTMENT
- WINGS 3 & 4
- ABUTMENT DETAILS
- PIER
- SUPERSTRUCTURE
- SUPERSTRUCTURE DETAILS
- SINGLE SLOPE PARAPET 32SS



4/20/2017

STRUCTURES DESIGN CONTACTS

BRIDGE OFFICE:
BILL DREHER (608) 266-8489
CONSULTANT:
BETH NEMEC (715) 342-3069
AECOM PROJECT NO. 60428202

BENCH MARK TABLE

NO.	STATION	DESCRIPTION	ELEVATION
BM 1	11+75.26, 54.84' RT	60d SPIKE IN PP AT THE SW CRNR. OF CTH V AND CEMETERY ROAD.	1163.59
BM 2	9+65.48, 16.22' RT	PAINTED MARK ON NW CORNER OF BRIDGE OVER JUMP RIVER.	1156.32
BM 3	6+64.42, 55.50' RT	60d SPIKE IN 14" ASPEN TREE ON THE SW CRNR. OF CTH V AND POPLAR ROAD.	1148.68

HORIZ. & VERT. TIED TO WIS. CO. COORDINATE SYSTEM, CHECKED TO MCKINLEY W. GPS
HORIZ. NAVD (88), VERT. NAD 83 (2011)

NO.	DATE	REVISION	BY
AECOM			
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
ACCEPTED	William C. Dreher SDR CHIEF STRUCTURES DESIGN ENGINEER		06/05/17 DATE
STRUCTURE B-54-125			
CTH V OVER LITTLE JUMP RIVER			
COUNTY	RUSK	TOWN/CITY/VILLAGE	MARSHALL
DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS			
DESIGNED BY	JAM	DESIGN CK'D. EAN	DRAWN BY KAM PLANS CK'D. MSK
GENERAL PLAN			SHEET 1 OF 12

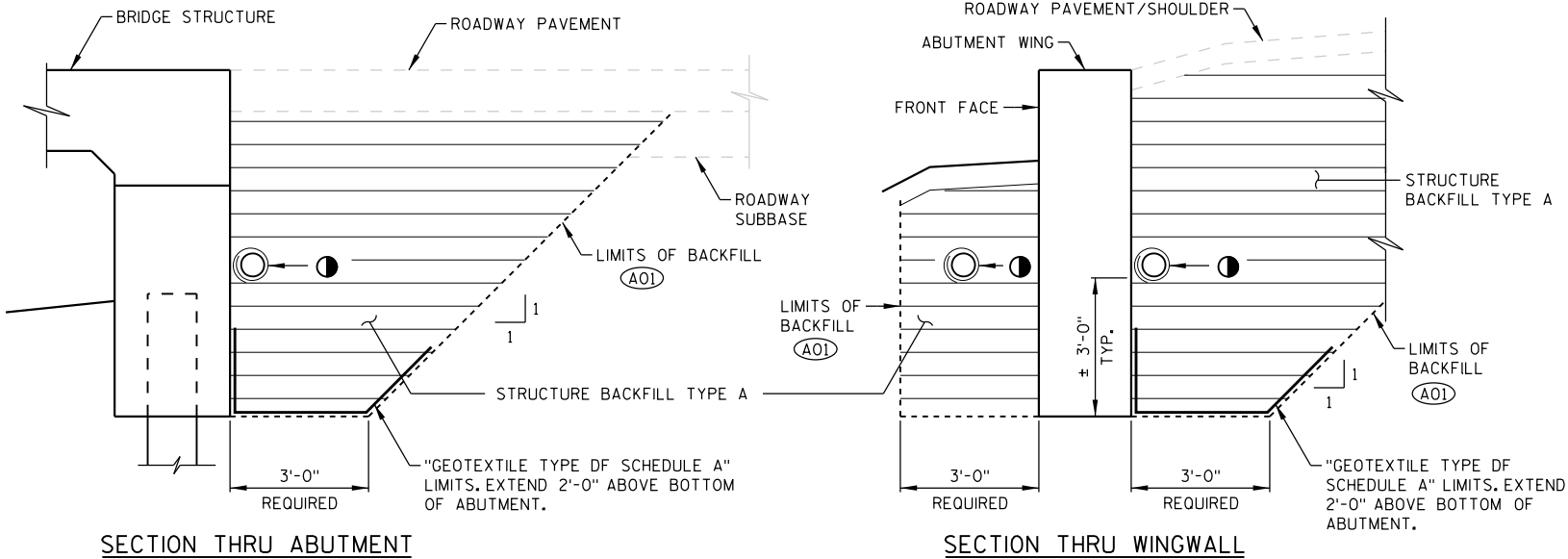
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BATCH PRINT SHEET 1 OF 1

8



BACKFILL STRUCTURE LIMITS

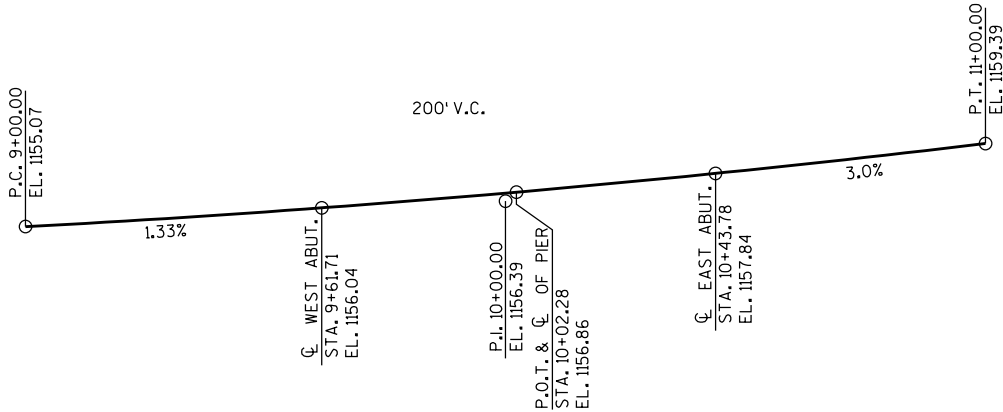
THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES BRIDGES B-54-125" SHALL BE THE EXISTING GROUNDLINE.

(AO1) BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

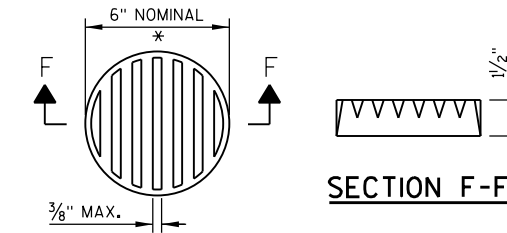
AT THE BACKFACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL.

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE THE BOTTOM OF THE ABUTMENT.

PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.



PROFILE GRADE LINE, CTH V

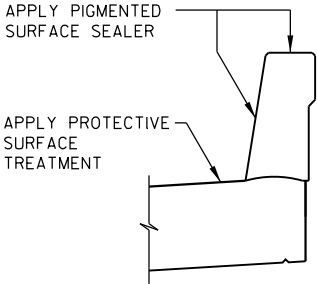


RODENT SHIELD DETAIL

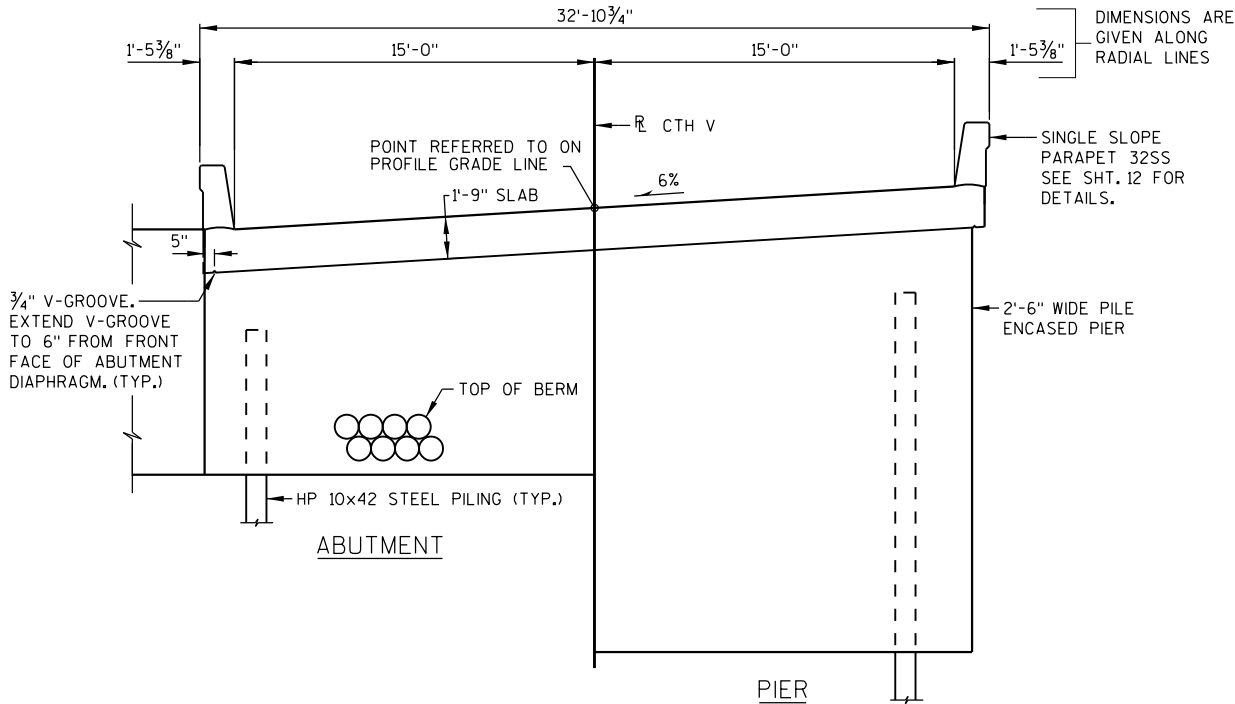
* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.



SURFACE PROTECTION DETAIL



CROSS SECTION THRU ROADWAY (LOOKING EAST)

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.

FILLER SHALL CONFORM TO THE REQUIREMENTS OF A.A.S.H.T.O. DESIGNATION M153, TYPES I, II OR III, OR M213.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH RIPRAP HEAVY AND GEOTEXTILE FABRIC TYPE 'HR' WITHIN THE LIMITS SHOWN ON SHEET 1, ON THE ABUTMENT SHEETS OR AS DIRECTED BY THE ENGINEER.

THE FIRST DIGIT OF A THREE DIGIT BAR MARK AND THE FIRST TWO DIGITS OF A FOUR DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

THE EXISTING STRUCTURE (P-54-131) IS A SINGLE SPAN PRESTRESSED CONCRETE CHANNEL BRIDGE, 60.8' LONG x 30.1' WIDE, TO BE REMOVED.

ALL REQUIRED REMOVAL OF THE EXISTING SUBSTRUCTURES IS INCLUDED IN THE BID ITEM "REMOVING OLD STRUCTURE OVER WATERWAY WITH MINIMAL DEBRIS, STATION 10+02.28."

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE UNLESS AN ALTERNATIVE METHOD IS APPROVED BY THE ENGINEER.

THE EXISTING GROUNDLINE SHALL BE THE UPPER LIMITS OF EXCAVATION FOR STRUCTURES.

EXCAVATION REQUIRED UNDER THE BID ITEM "EXCAVATION FOR STRUCTURES BRIDGES B-54-125" IS NOT USED TO BALANCE THE EARTHWORK.

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEM	UNIT	WEST ABUTMENT	PIER	EAST ABUTMENT	SUPER.	TOTALS
203.0600.S.01	REMOVING OLD STRUCUTRE OVER WATERWAY WITH MINIMAL DEBRIS STATION 10+02.28	LS					1
206.1000	EXCAVATION FOR STRUCTURES BRIDGES B-54-125	LS					1
210.1500	BACKFILL STRUCTURE TYPE A	TON	370		370		740
502.0100	CONCRETE MASONRY BRIDGES	CY	54	50	56	205	365
502.3200	PROTECTIVE SURFACE TREATMENT	SY				285	285
502.3210	PIGMENTED SURFACE SEALER	SY				75	75
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	2,840	2,520	2,840		8,200
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	1,955	70	1,955	50,870	54,850
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	7		7		14
550.1100	PIILING STEEL HP 10-INCH X 42 LB	LF	240	210	240		690
606.0300	RIPRAP HEAVY	CY	130		120		250
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	100		100		200
614.0150	ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD	EACH	2		2		4
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	56		56		112
645.0120	GEOTEXTILE TYPE HR	SY	230		215		445
	NON-BID ITEMS						
	FILLER	SIZE					1/2" & 3/4"

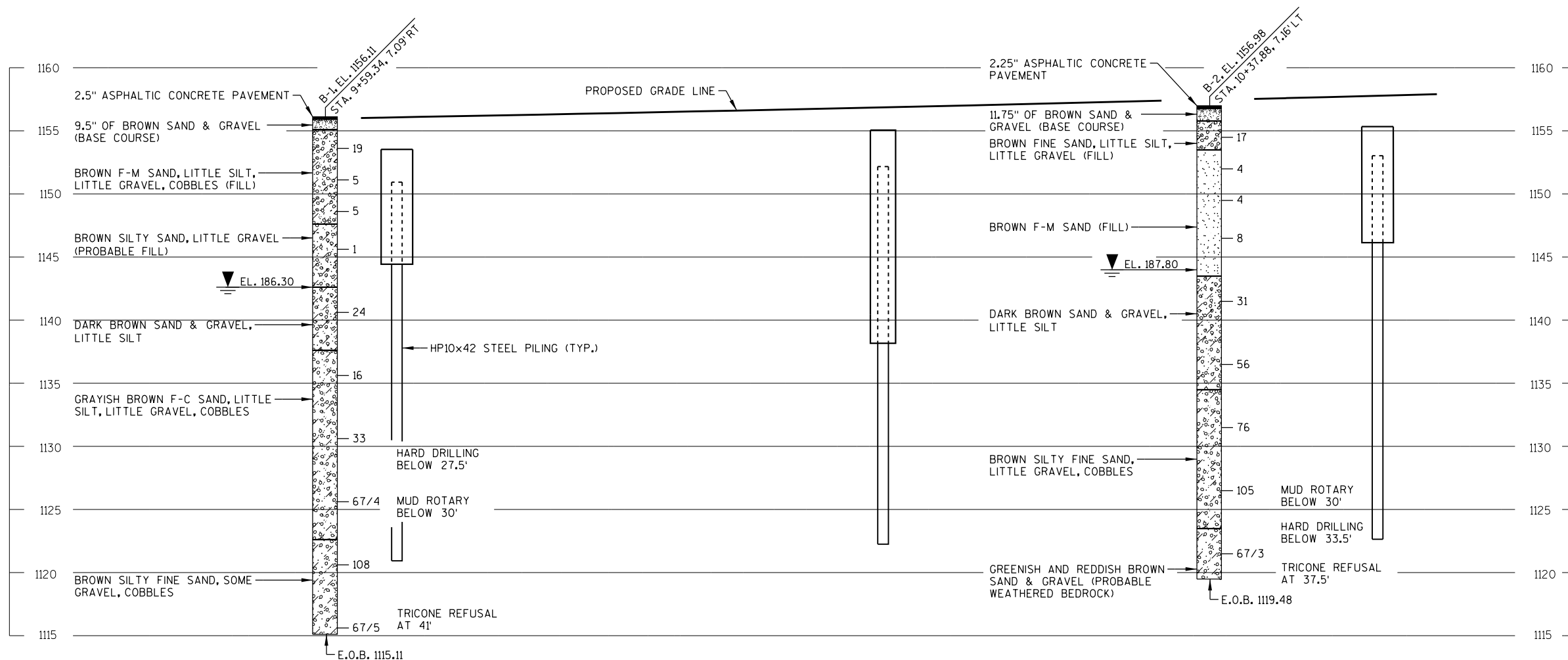
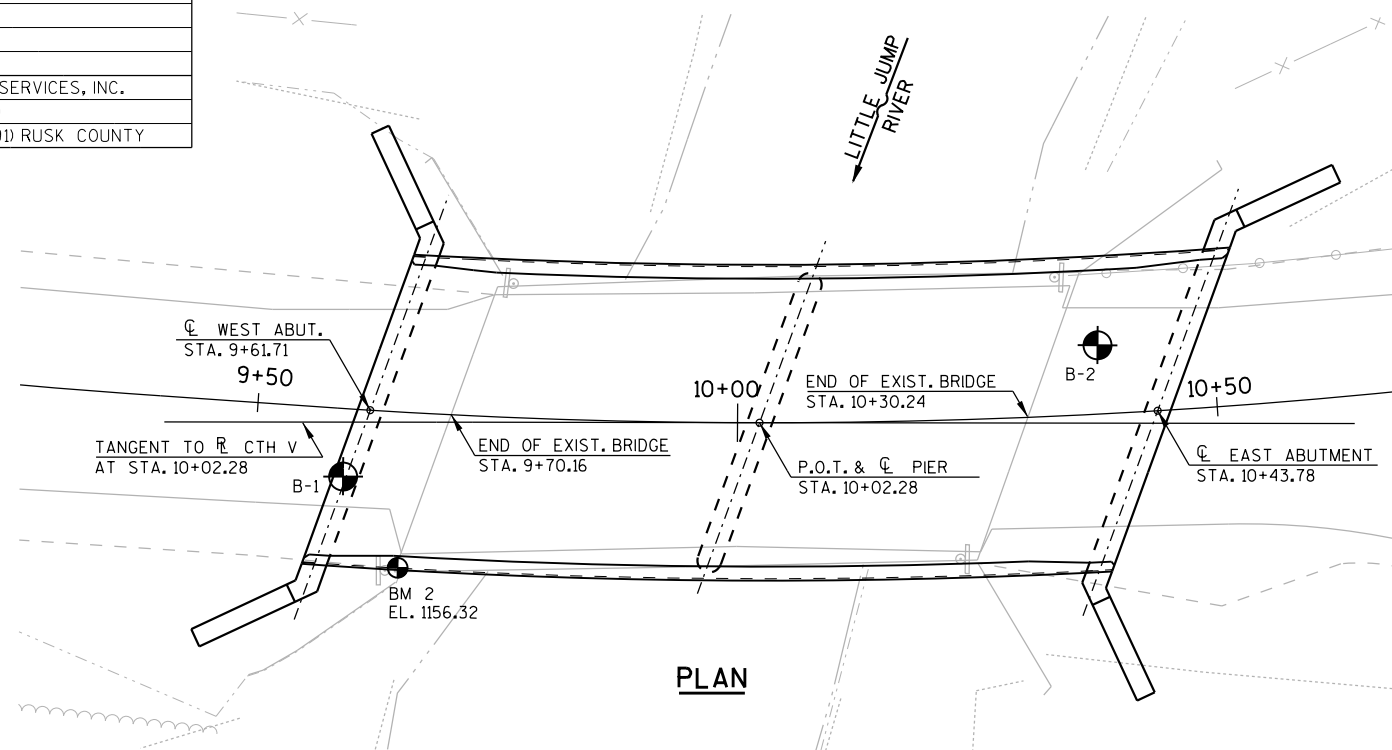
STATE PROJECT NUMBER
















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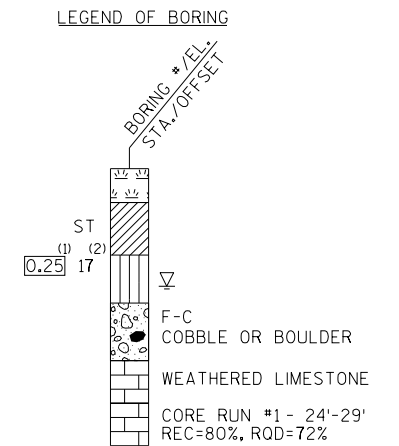
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY		KAM	PLANS CK'D. MSK
CROSS SECTION AND QUANTITIES			SHEET 2 OF 12

8

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
B-1	JUNE 18, 2015	526970.02	848166.31
B-2	JUNE 18, 2015	526948.99	848243.26
BORINGS COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
REPORT COMPLETED BY: 7/13/2015 (REVISED)			
ALL COORDINATES REFERENCED TO WCCS NAD 83(91) RUSK COUNTY			



STATE PROJECT NUMBER			
8796-00-70			
MATERIAL SYMBOLS			
	ASPHALT		TOPSOIL
	CONCRETE		FILL
	SAND		CLAY
	BOULDERS OR COBBLES		LIMESTONE
	SHALE		SANDSTONE
			PEAT
			GRAVEL
			SILT
			BEDROCK (UNKNOWN)
			IGNEOUS/ META



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

▽ AT TIME OF DRILLING

▼ END OF DRILLING

▼ AFTER DRILLING

ABBREVIATIONS

F-FINE	M-MEDIUM	C-COARSE	ST-SHELBY TUBE
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SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION			
STRUCTURE B-54-125			
DRAWN BY		KAM	PLANS CK'D. MSK
SUBSURFACE EXPLORATION		SHEET 3 OF 12	

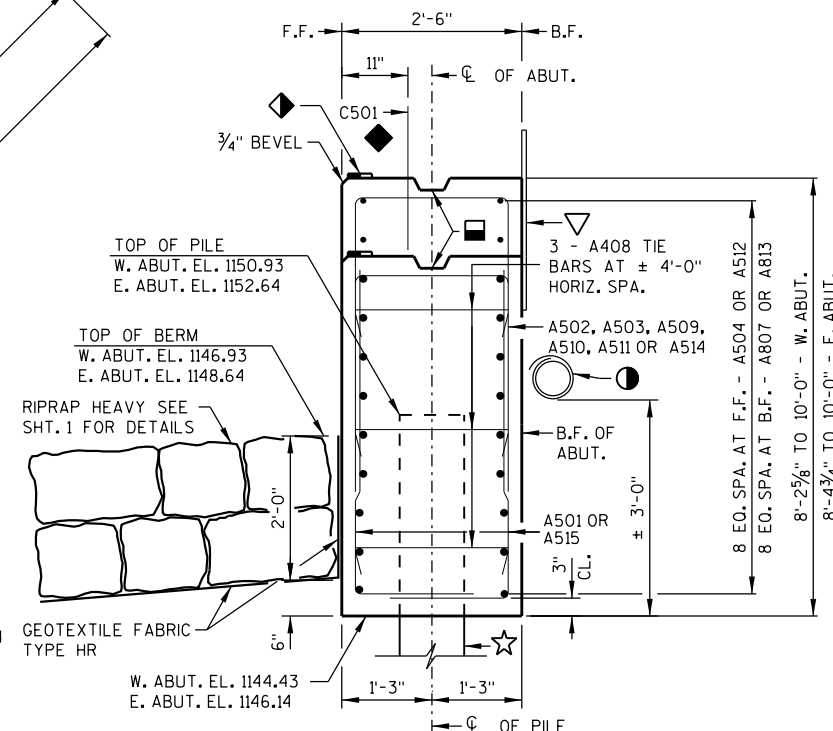
ELEVATIONS AND DIMENSIONS ARE GIVEN AT THE C OF ABUTMENT. FOR
WING DETAILS AND ELEVATIONS SEE SHEETS 5 & 7.

LEGEND

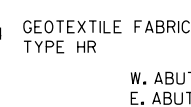
- ☆ SUPPORT ABUTMENTS ON PILING STEEL HP 10-INCH x 42 LB. SEE FOUNDATION DATA ON SHEET 1 AND PILE SPlice DETAIL ON SHEET 8.
- PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. DRAIN BOTH ABUTMENTS TO DOWNSTREAM SIDE OF BRIDGE. ATTACH RODENT SHIELD AT ENDS OF PIPE. SEE SHT. 2.
- 1/2" FILLER TO EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE.)
- ◆ C501 OR P511 AT 1'-0". COATED. BARS MAY BE PLACED AFTER CONCRETE HAS BEEN POURED BUT PRIOR TO ITS INITIAL SET. EMBED 1'-0".
- ▽ 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
- ◀ 4"x3/4" FILLER - TO EXTEND BETWEEN WING WALLS.
- ◻ KEYED CONSTRUCTION JOINT FORMED BY BEVELED 2"x6".
- ◻ OPTIONAL CONSTRUCTION JOINT KEYWAY FORMED BY A BEVELED 2"x6", WITH RUBBERIZED MEMBRANE WATERPROOFING ON BACKFACE.
(COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY BRIDGES")



ELEVATION
(LOOKING WEST AT WEST ABUTMENT)
37'-11 $\frac{3}{8}$ "



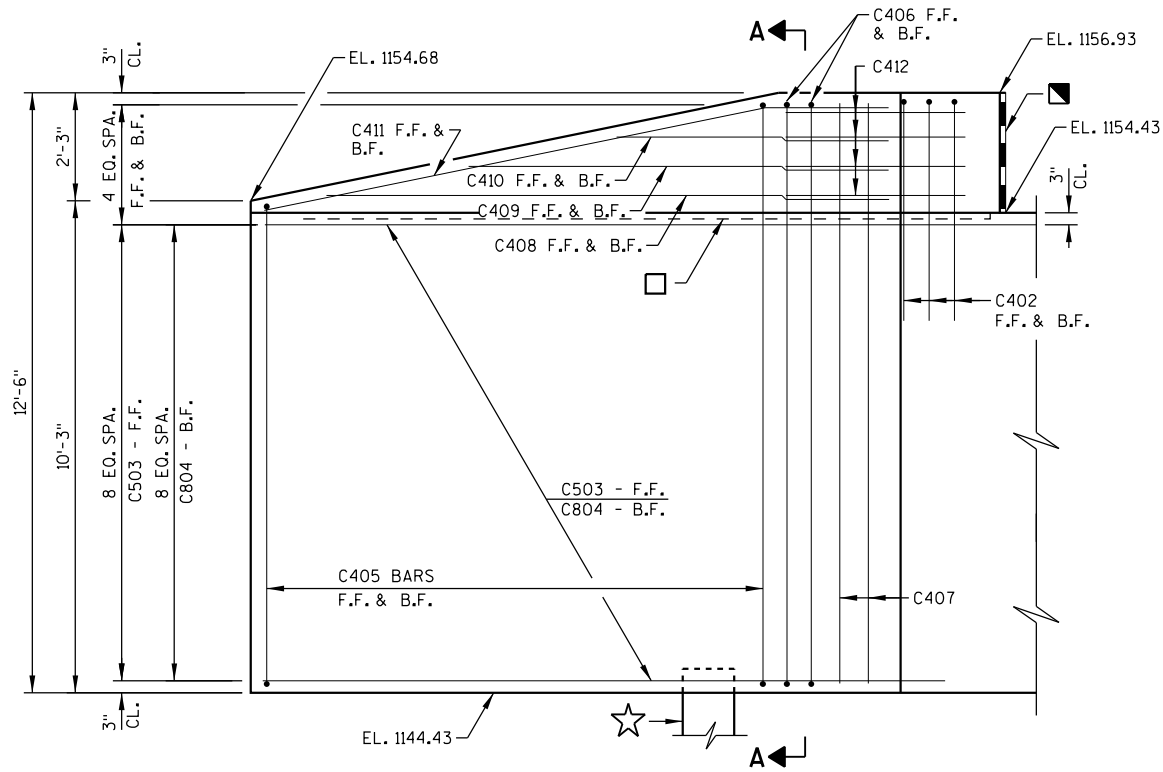
TYPICAL SECTION THRU BODY



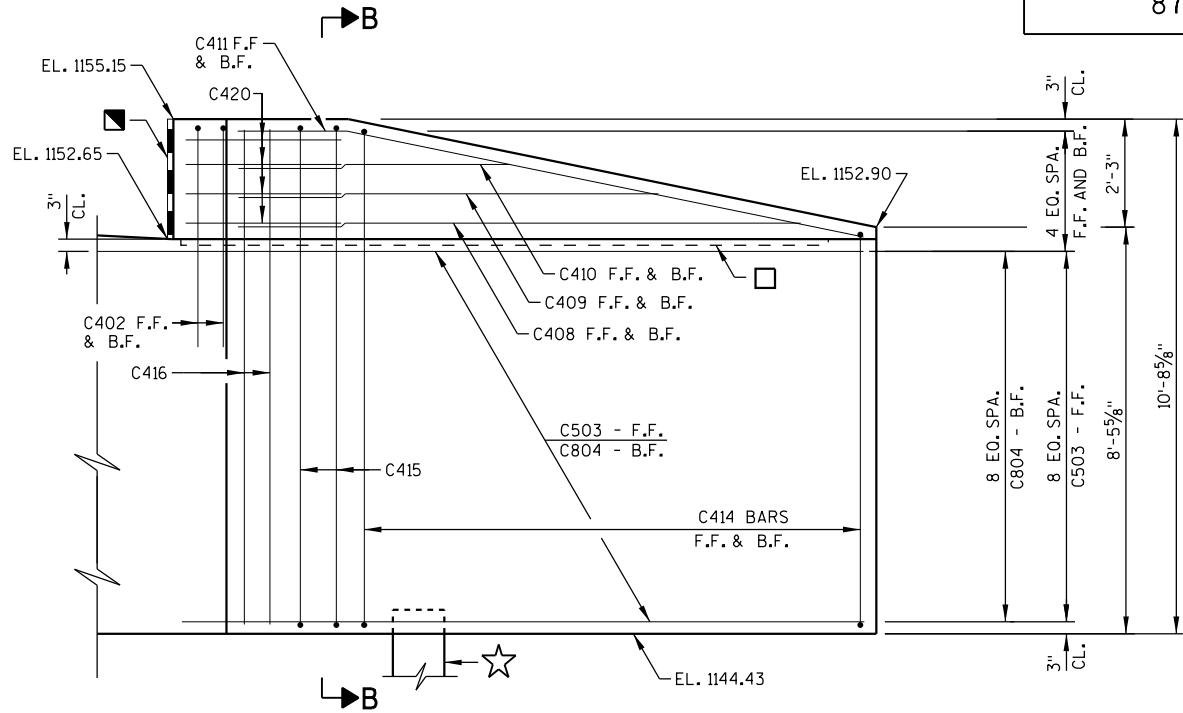
PILE PLAN

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE		B-54-125	
DRAWN BY		KAM	PLANS CK'D. MSK
WEST ABUTMENT		SHEET 4 OF 12	

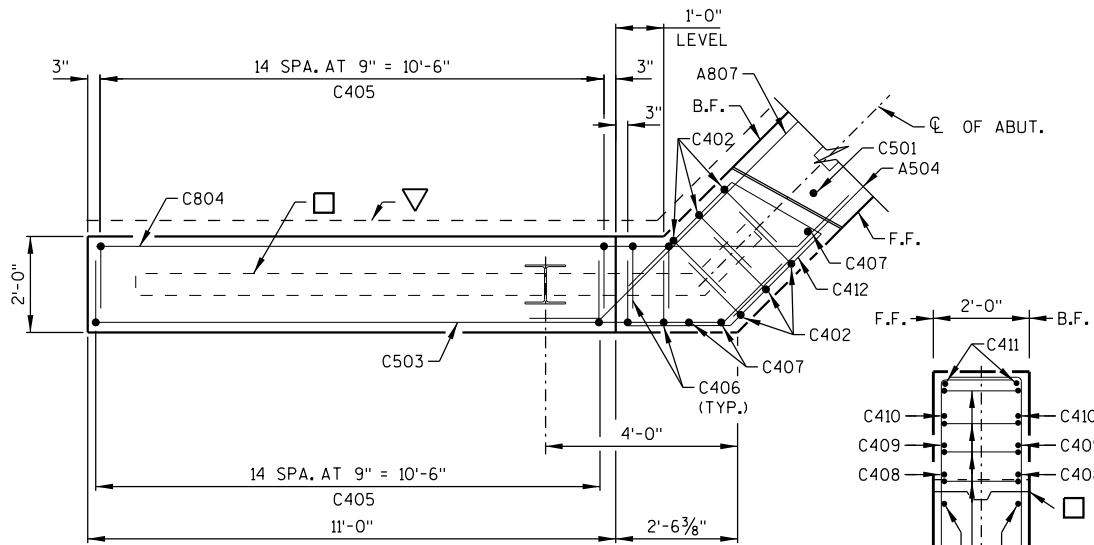
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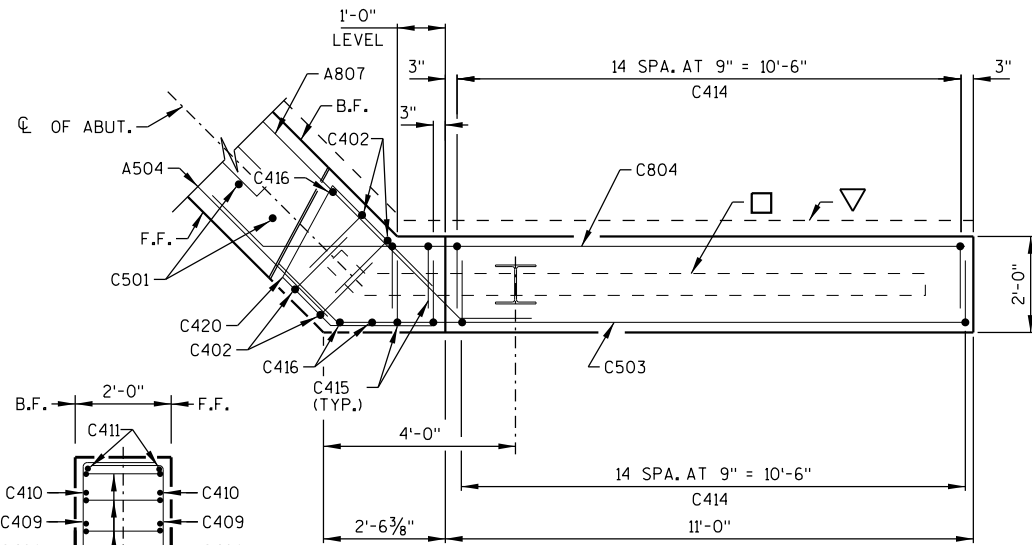
ELEVATION - WING 1



ELEVATION - WING 2



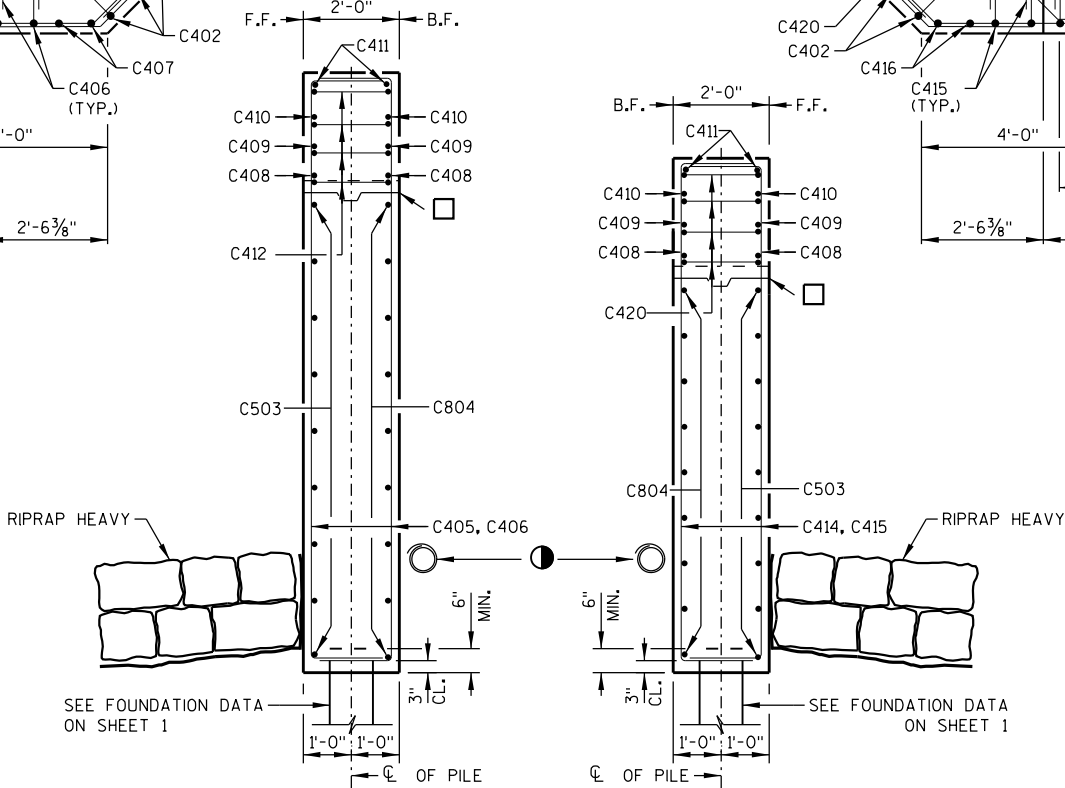
PLAN - WING 1



PLAN - WING 2

LEGEND

- ☆ SUPPORT ABUTMENTS ON PILING STEEL HP 10-INCH x 42 LB. SEE FOUNDATION DATA ON SHEET 1 AND PILE SPLICE DETAIL ON SHEET 8.
- PIPE UNDERDRAIN WRAPPED (6-INCH), SLOPE 0.5% MIN. TO SUITABLE DRAINAGE, DRAIN BOTH ABUTMENTS TO DOWNSTREAM SIDE OF BRIDGE. ATTACH RODENT SHIELD AT ENDS OF PIPE. SEE SHT. 2.
- 1/2" FILLER TO EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE.)
- ▽ 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
- OPTIONAL CONSTRUCTION JOINT KEYWAY FORMED BY A BEVELED 2"x6", WITH RUBBERIZED MEMBRANE WATERPROOFING ON BACKFACE. (COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY BRIDGES")



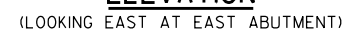
SECTION A-A

SECTION B-B

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY KAM		PLANS CK'D. MSK	
WINGS 1 & 2			SHEET 5 OF 12

SEE SHEET 4 FOR "TYPICAL SECTION THRU BODY"

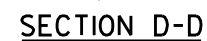
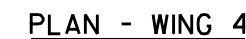
- ☆ SUPPORT ABUTMENTS ON PILING STEEL HP 10-INCH x 42 LB. SEE FOUNDATION DATA ON SHEET 1 AND PILE SPLICE DETAIL ON SHEET 8.
- PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. DRAIN BOTH ABUTMENTS TO DOWNSTREAM SIDE OF BRIDGE. ATTACH RODENT SHIELD AT ENDS OF PIPE. SEE SHT. 2.
- 1/2" FILLER TO EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE.)
- ◆ C501 OR P511 AT 1'-0". COATED. BARS MAY BE PLACED AFTER CONCRETE HAS BEEN POURED BUT PRIOR TO ITS INITIAL SET. EMBED 1'-0".
- ▽ 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
- ◀ 4"x3 3/4" FILLER - TO EXTEND BETWEEN WING WALLS.
- ◻ KEYED CONSTRUCTION JOINT FORMED BY BEVELED 2"x6".
- ◻ OPTIONAL CONSTRUCTION JOINT KEYWAY FORMED BY A BEVELED 2"x6", WITH RUBBERIZED MEMBRANE WATERPROOFING ON BACKFACE.
(COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY BRIDGES")



NOTE: SPACE A510, A511, A514 & A515 TO MISS PILINGS.



NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE		B-54-125	
DRAWN BY		KAM	PLANS CKD. MSK
EAST ABUTMENT		SHEET 6 OF 12	



- ## LEGEND
- ☆ SUPPORT ABUTMENTS ON PILING STEEL HP 10-INCH x 42 LB. SEE FOUNDATION DATA ON SHEET 1 AND PILE SPLICE DETAIL ON SHEET 8.
 - PIPE UNDERDRAIN WRAPPED (6-INCH), SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. DRAIN BOTH ABUTMENTS TO DOWNSTREAM SIDE OF BRIDGE. ATTACH RODENT SHIELD AT ENDS OF PIPE. SEE SHT. 2.
 - 1/2" FILLER TO EXTEND AS SHOWN. SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE.)
 - ▽ 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
 - OPTIONAL CONSTRUCTION JOINT KEYWAY FORMED BY A BEVELED 2"x6", WITH RUBBERIZED MEMBRANE WATERPROOFING ON BACKFACE.
(COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY BRIDGES")

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY		KAM	PLANS CK'D. MS
WINGS 3 & 4			SHEET 7 OF 1

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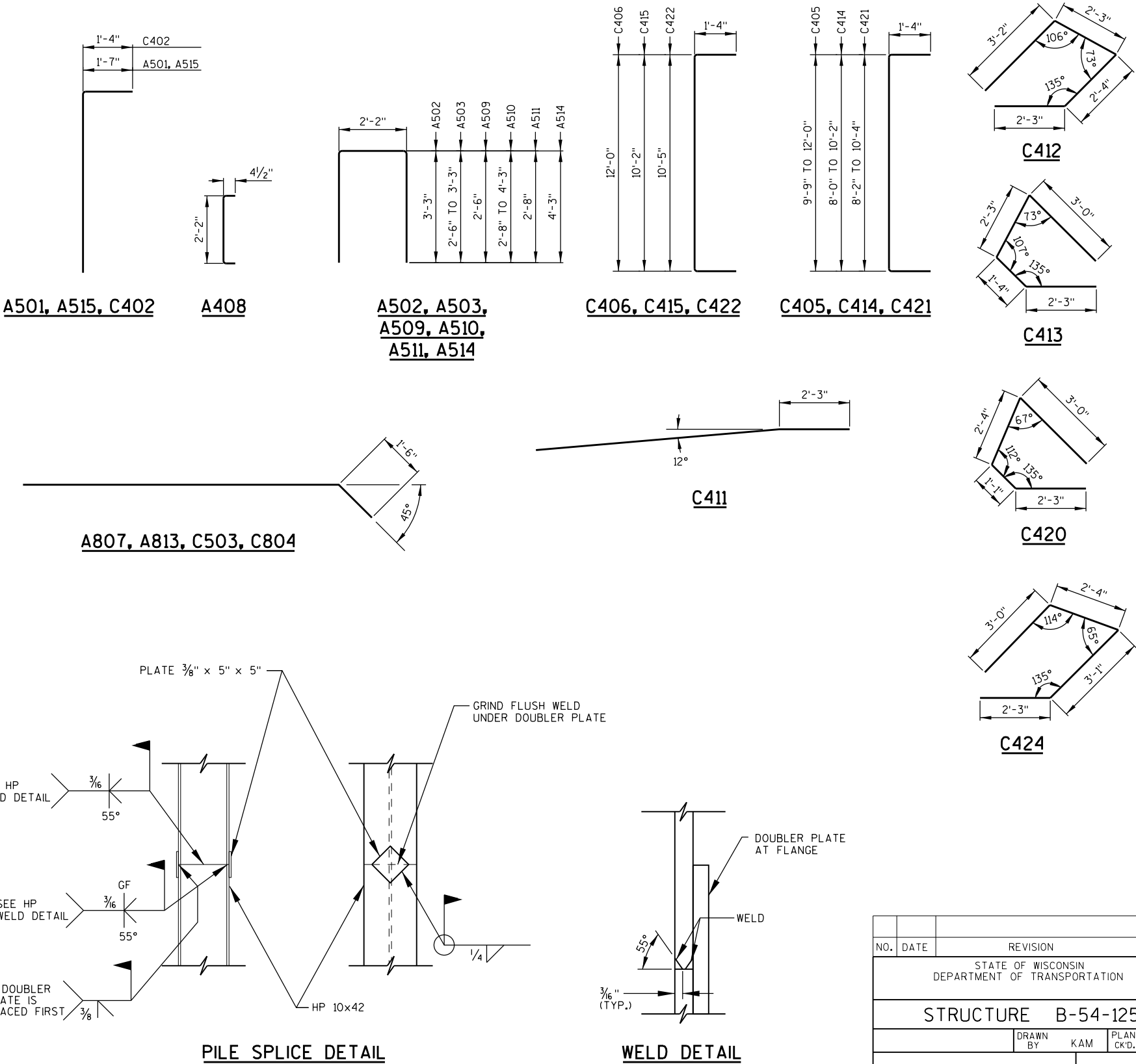
BAR SERIES

MARK	NO. REQ'D	LENGTH
A503	1 SERIES OF 33	6'-11" TO 8'-5"
A510	1 SERIES OF 35	7'-3" TO 10'-5"
C405	4 SERIES OF 15	12'-3" TO 14'-6"
C414	2 SERIES OF 15	10'-6" TO 12'-8"
C421	2 SERIES OF 15	10'-8" TO 12'-10"

BILL OF BARS

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.
Δ LENGTH SHOWN FOR BAR IS AN AVG. LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.
BOTH ABUTMENTS INCLUDED IN THIS BILL OF BARS.

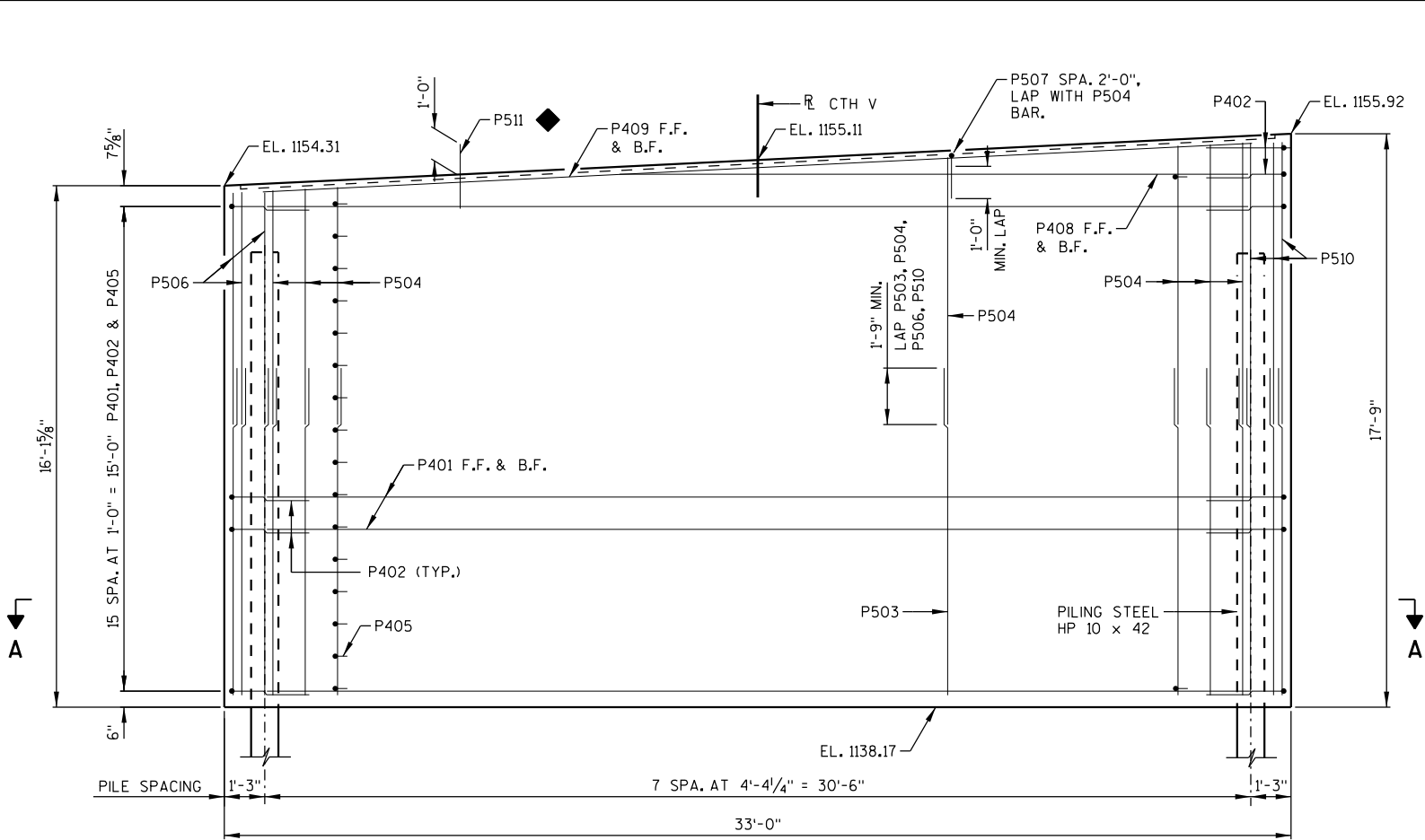
MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
NON-COATED BARS					TOTAL WEIGHT = 5,600 LBS
A501	78	9-2	X		W. ABUTMENT BODY - F.F. & B.F. VERT.
A502	3	8-5	X		W. ABUTMENT BODY - TOP VERT.
A503	33	7-8	X	Δ	W. ABUTMENT BODY - TOP VERT.
A504	18	20-4			W. ABUTMENT BODY - F.F. HORIZ.
A_05					NOT USED
A_06					NOT USED
A807	18	25-5	X		W. ABUTMENT BODY - B.F. HORIZ.
A408	66	2-9	X		ABUTMENT BODY - TIE HORIZ.
A509	3	6-11	X		W. ABUTMENT BODY - TOP VERT.
A510	35	8-10	X	Δ	E. ABUTMENT BODY - TOP VERT.
A511	3	7-3	X		E. ABUTMENT BODY - TOP VERT.
A512	18	21-2			E. ABUTMENT BODY - F.F. HORIZ.
A813	18	26-3	X		E. ABUTMENT BODY - B.F. HORIZ.
A514	3	10-5	X		E. ABUTMENT BODY - TOP VERT.
A515	80	9-3	X		E. ABUTMENT BODY - F.F. & B.F. VERT.
COATED BARS					TOTAL WEIGHT = 3,910 LBS
C501	68	2-0			ABUTMENT BODY - DOWEL TO SLAB VERT.
C402	20	4-7	X		WING 1,2,3,4 - F.F. & B.F. VERT.
C503	36	14-8	X		WING 1, 2, 3 & 4 - F.F. HORIZ.
C804	36	16-5	X		WING 1, 2, 3 & 4 - B.F. HORIZ.
C405	60	13-5	X	Δ	WING 1 & 4 - F.F. & B.F. VERT.
C406	8	14-6	X		WING 1 & 4 - F.F. & B.F. VERT.
C407	6	12-0			WING 1 & 4 - F.F. & B.F. VERT.
C408	8	12-2			WING 1, 2, 3 & 4 - F.F. & B.F. HORIZ.
C409	8	8-10			WING 1, 2, 3 & 4 - F.F. & B.F. HORIZ.
C410	8	5-6			WING 1, 2, 3 & 4 - F.F. & B.F. HORIZ.
C411	8	12-3	X		WING 1, 2, 3 & 4 - F.F. & B.F. HORIZ.
C412	8	9-9	X		WING 1 - TIE HORIZ.
C413	4	8-7	X		WING 4 - TIE HORIZ.
C414	30	11-7	X	Δ	WING 2 - F.F. & B.F. VERT.
C415	4	12-8	X		WING 2 - F.F. & B.F. VERT.
C416	3	10-2			WING 2 - F.F. & B.F. VERT.
C_17					NOT USED
C_18					NOT USED
C_19					NOT USED
C420	4	8-5	X		WING 2 - TIE HORIZ.
C421	30	11-9	X	Δ	WING 3 - F.F. & B.F. VERT.
C422	4	12-11	X		WING 3 - F.F. & B.F. VERT.
C423	3	10-5			WING 3 - F.F. VERT.
C424	4	10-5	X		WING 3 - TIE HORIZ.



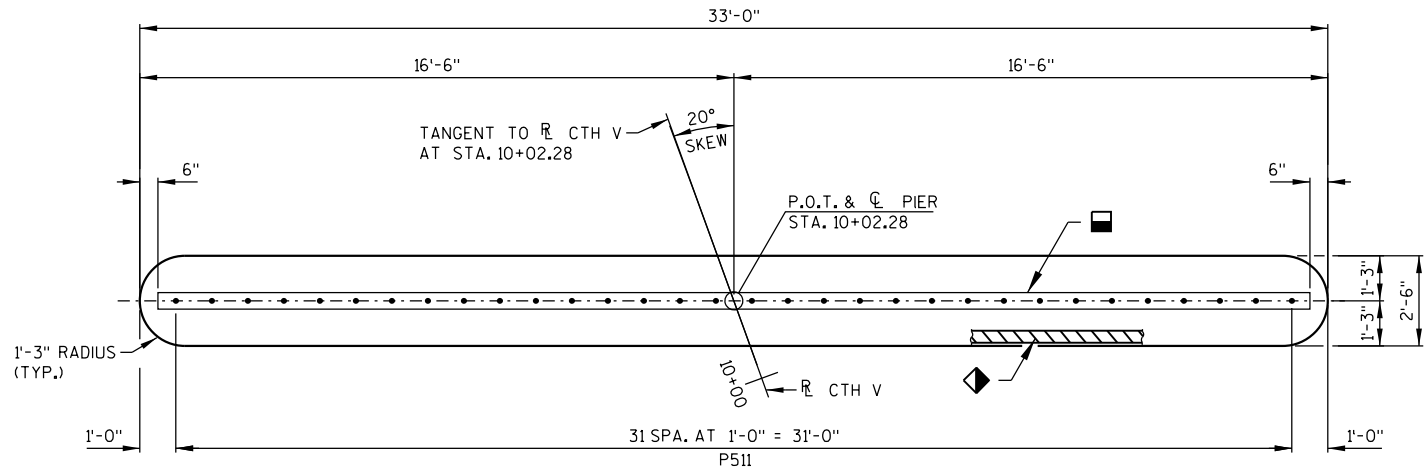
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY		KAM	PLANS CK'D. MSK
ABUTMENT DETAILS		SHEET 8 OF 12	

8

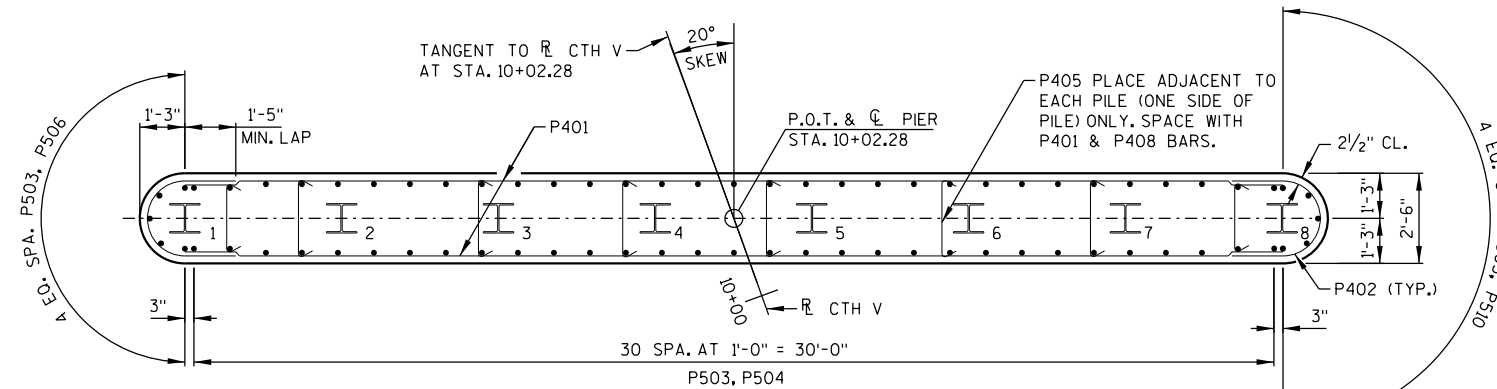
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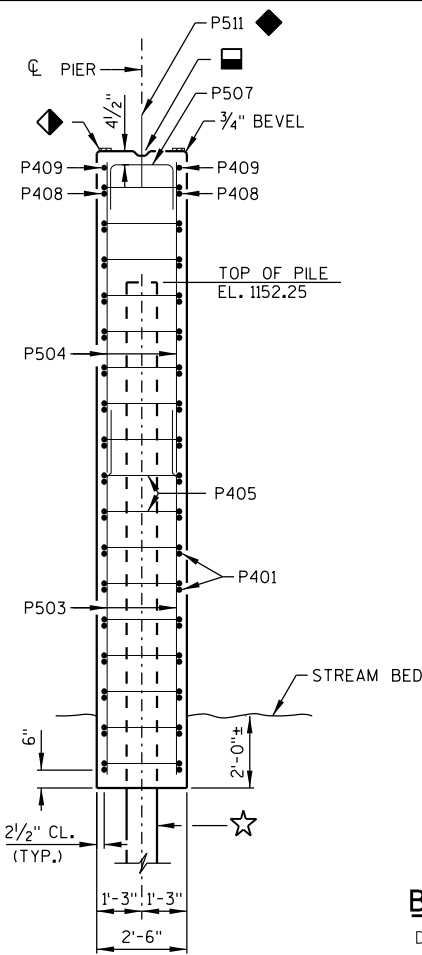
ELEVATION
LOOKING UPSTATION



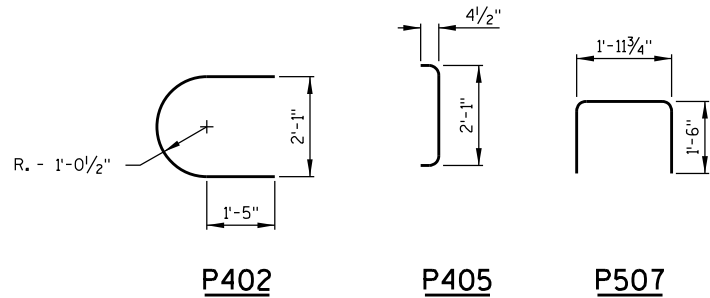
PLAN



SECTION A-A



TYPICAL SECTION
THRU PIER



BILL OF BARS

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.
Δ LENGTH SHOWN FOR BAR IS AN AVG. LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

MARK	NO.	REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
NON-COATED BARS						TOTAL WEIGHT = 2,520 LBS
P401	32		30 - 6			PIER BODY HORIZ.
P402	34		6 - 2	X		PIER BODY AT ENDS HORIZ.
P503	72		10 - 0			PIER BODY AT BOTTOM VERT.
P504	62		7 - 10		Δ	PIER BODY AT TOP VERT.
P405	132		2 - 8	X		PIER BODY TIES AT PILES HORIZ.
P506	5		7 - 0			PIER BODY AT ENDS VERT.
P507	16		4 - 9	X		PIER BODY AT TOP HORIZ.
P408	2		19 - 6			PIER BODY AT TOP HORIZ.
P409	2		30 - 7			PIER BODY AT TOP HORIZ.
P510	5		8 - 8			PIER BODY AT ENDS VERT.
COATED BARS						TOTAL WEIGHT = 70 LBS
P511	32		2 - 0			PIER BODY - DOWELS VERT.

BAR SERIES

MARK	NO.	REQ'D	LENGTH
P504	2	SERIES OF 31	7'-1" TO 8'-7"

LEGEND

FOR SYMBOL DESCRIPTIONS SEE SHT. 4

NOTES

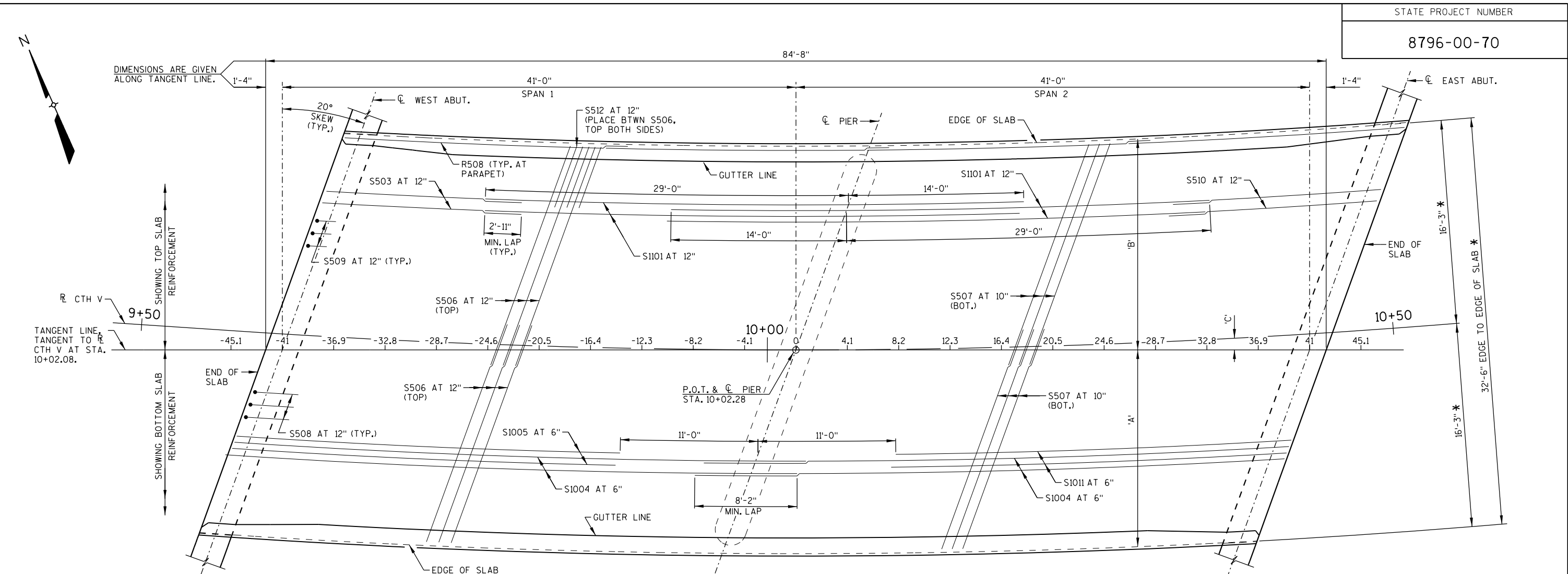
EXCAVATE TO EL. 1138.17 BEFORE DRIVING PILING. EXCAVATION IS INCIDENTAL TO BID ITEM "EXCAVATION FOR STRUCTURES B-54-125"

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY KAM		PLANS CK'D. MSK	
PIER		SHEET 9 OF 12	

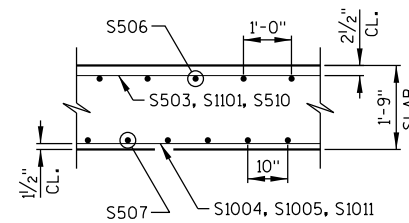
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PLOT TIME: 12:00:30 PM

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BATCH PRINT SHEET 10 OF 12



PLAN



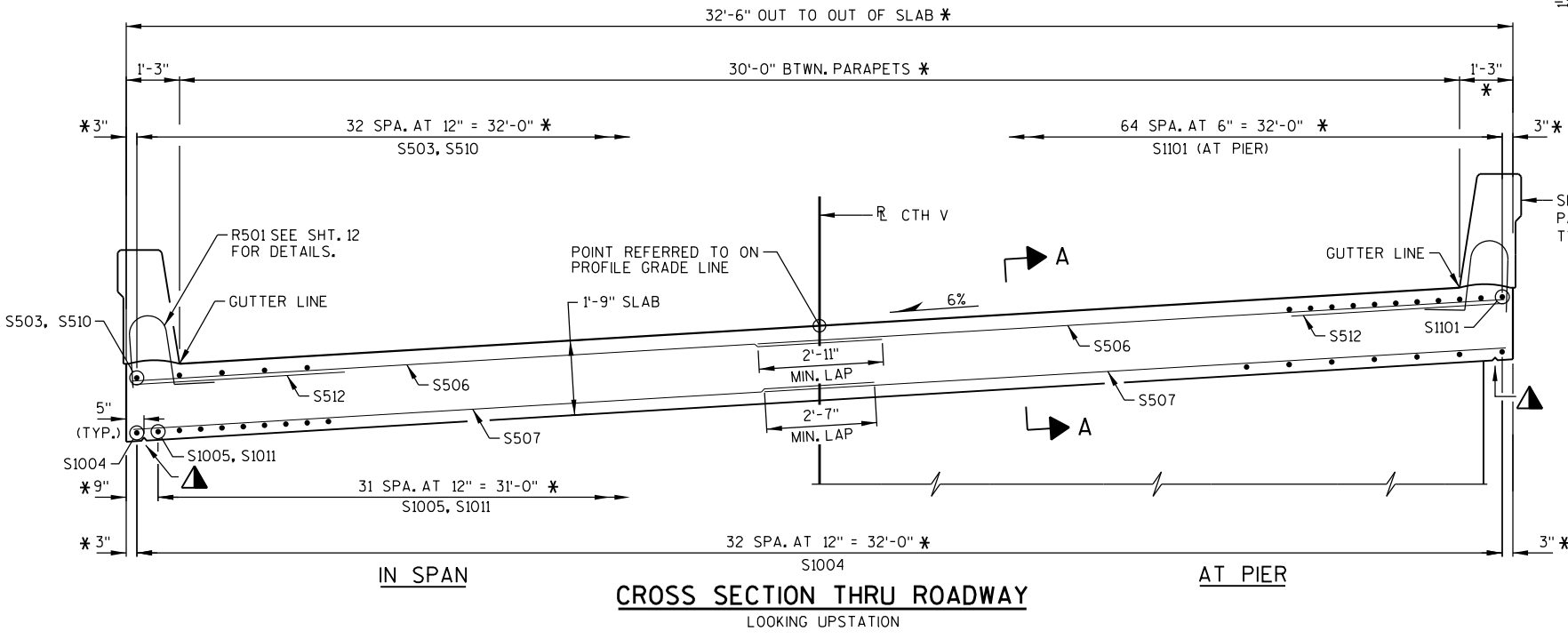
SECTION A - A

NOTES

- ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).
- CAMBER SPAN AS SHOWN TO PROVIDE FOR DEADLOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT.
- TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.
- PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE CL OF ABUTMENTS, CL OF PIER AND AT 5/10 PT. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG EDGE OF DECK AND CROWN OR CL.
- PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE TOP OF DECK SURFACE.
- SEE SHT. 12 'SINGLE SLOPE PARAPET 32SS' FOR PARAPET REINFORCEMENT PLACED IN SLAB.
- LAP R508 BARS IN SLOPED FACE PARAPET 32SS 1'-9" MIN.
- PARAPET SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED.
- ALL TRANSVERSE BAR STEEL REINFORCEMENT SHALL BE PLACED ALONG THE SKEW.
- ALL LONGITUDINAL BAR STEEL REINFORCEMENT SHALL BE PLACED CONCENTRIC TO CL BRIDGE.

LEGEND

- 3/4" CONTINUOUS V-GROOVE, TERMINATE 6" FROM FRONT FACE OF ABUTMENTS.
- * MEASURED RADIAL TO CL BRIDGE.



CROSS SECTION THRU ROADWAY
LOOKING UPSTATION

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY		KAM	PLANS CK'D. MSK
SUPERSTRUCTURE		SHEET 10 OF 12	

8

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BATCH PRINT SHEET 11 OF 12

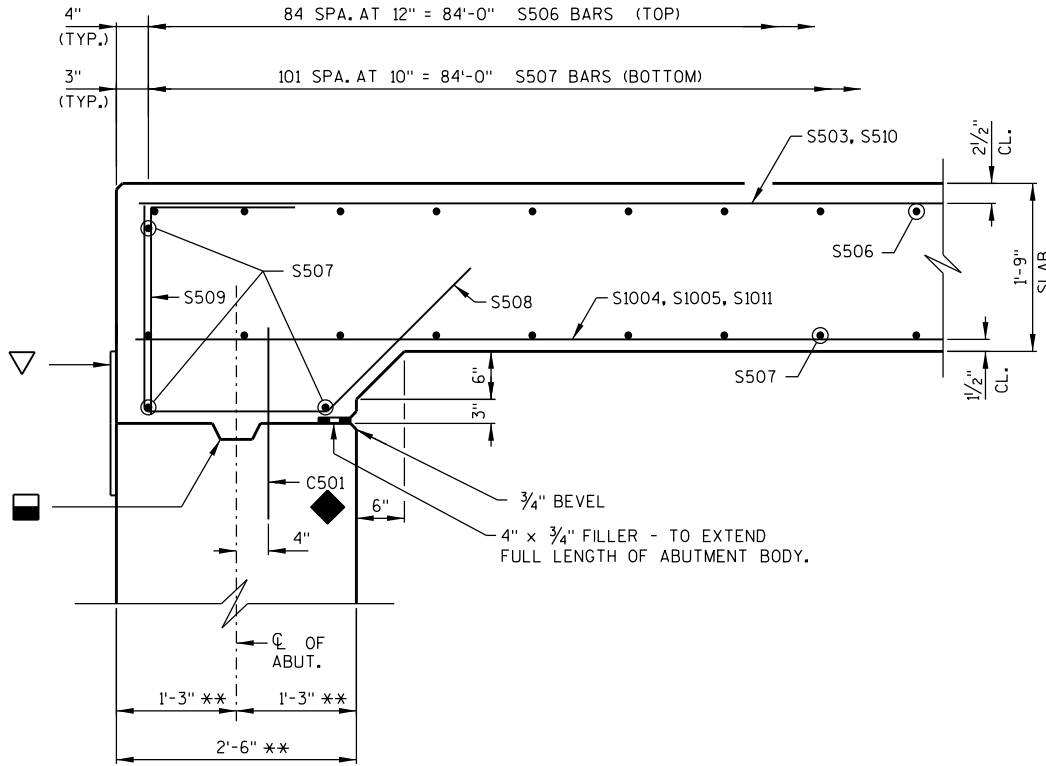
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TOP OF DECK ELEVATIONS & OFFSETS

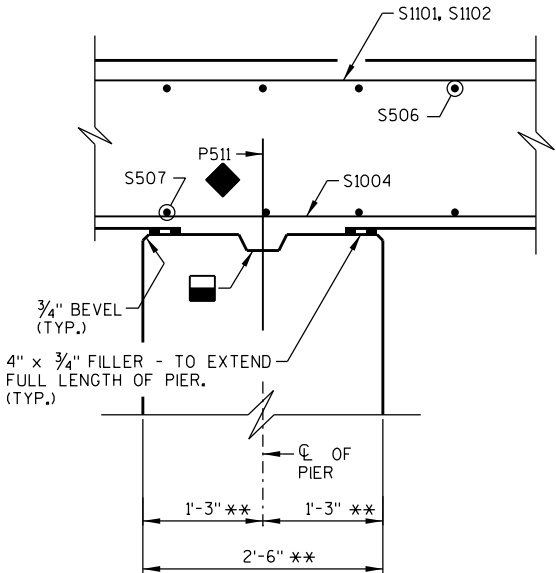
		C.L. BRG. W. ABUT.		1/10 PT.	2/10 PT.	3/10 PT.	4/10 PT.	5/10 PT.	6/10 PT.	7/10 PT.	8/10 PT.	9/10 PT.	C.L. BRG. PIER		1/10 PT.	2/10 PT.	3/10 PT.	4/10 PT.	5/10 PT.	6/10 PT.	7/10 PT.	8/10 PT.	9/10 PT.	C.L. BRG. E. ABUT.	
DISTANCE	-45.1	-41	-36.9	-32.8	-28.7	-24.6	-20.5	-16.4	-12.3	-8.2	-4.1	-4.1	0	4.1	8.2	12.3	16.4	20.5	24.6	28.7	32.8	36.9	41	45.1	
OFFSET "A"	14.74	15.01	15.24	15.47	15.64	15.8	15.94	16.05	16.14	16.2	16.24	16.25	16.24	16.2	16.14	16.05	15.94	15.8	15.64	15.45					
ELEV. "A"	1155.98	1156.05	1156.13	1177.21	1156.28	1156.36	1156.44	1156.52	1156.60	1156.69	1156.77	1156.86	1156.95	1157.04	1157.13	1157.22	1157.31	1157.41	1157.50	1157.60					
OFFSET "B"				17.09	16.89	16.72	16.58	16.46	16.36	16.3	16.26	16.25	16.26	16.3	16.37	16.46	16.58	16.72	16.89	17.09	17.31	17.56	17.83		
ELEV. "B"				1156.17	1156.25	1156.34	1156.42	1156.50	1156.59	1156.68	1156.77	1156.86	1156.95	1157.05	1157.14	1157.24	1157.34	1157.44	1157.54	1157.64	1157.74	1157.85	1157.96		
OFFSET "C"		1.27	1.03	0.82	0.62	0.46	0.32	0.2	0.11	0.05	0.01	0	0.01	0.05	0.11	0.2	0.32	0.46	0.62	0.82	1.03	1.27			
ELEV. "C"		1156.04	1156.11	1156.19	1156.27	1156.35	1156.43	1156.51	1156.60	1156.68	1156.77	1156.86	1156.95	1157.04	1157.13	1157.23	1157.32	1157.42	1157.52	1157.62	1157.72	1157.82			

OFFSET 'A' - (RT. EDGE OF DECK)
 OFFSET 'B' - (LT. EDGE OF DECK)
 OFFSET 'C' - (R. CTH V)

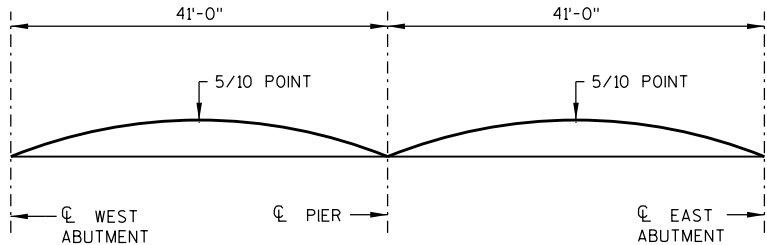
▲ MEASURED ALONG TANGENT LINE, TANGENT TO R. CTH V AT STA. 10+02.28



ABUTMENT DETAIL



PIER DETAIL



CAMBER DIAGRAM

SPAN 1	C. BRG. W. ABUT.	1/10 PT.	2/10 PT.	3/10 PT.	4/10 PT.	5/10 PT.	6/10 PT.	7/10 PT.	8/10 PT.	9/10 PT.	C. BRG. PIER
CAMBER (IN.)	0.0	0.3	0.5	0.7	0.8	0.8	0.6	0.5	0.3	0.1	0.0
SPAN 2	C. BRG. PIER	1/10 PT.	2/10 PT.	3/10 PT.	4/10 PT.	5/10 PT.	6/10 PT.	7/10 PT.	8/10 PT.	9/10 PT.	C. BRG. E. ABUT.
CAMBER (IN.)	0.0	0.1	0.3	0.5	0.6	0.8	0.8	0.7	0.5	0.3	0.0

BILL OF BARS

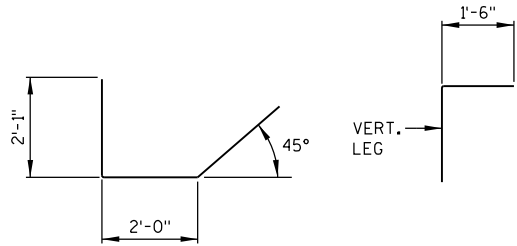
DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.
 ALL BAR STEEL REINFORCEMENT SHOWN IN THIS BILL SHALL BE EPOXY COATED.

MARK	NO.	REQ'D.	LENGTH	BENT	LOCATION	TOTAL WEIGHT = 46,950 LBS
S1101	65	43 - 0			SLAB - TOP	LONGIT.
S503	33	15 - 9			SLAB - TOP	LONGIT.
S1004	66	46 - 5			SLAB - BOTTOM	LONGIT.
S1005	32	30 - 9			SLAB - BOTTOM	LONGIT.
S506	170	19 - 1			SLAB - TOP	TRANS.
S507	216	18 - 11			SLAB - BOTTOM	TRANS.
S508	66	6 - 0	X		SLAB - TIES AT ABUTMENT	LONGIT.
S509	66	3 - 6	X		SLAB - TIES AT ABUTMENT	LONGIT.
S510	33	16 - 9			SLAB - TOP	LONGIT.
S1011	32	31 - 10			SLAB - BOTTOM	LONGIT.
S512	168	5 - 0			SLAB - EDGE	TRANS.

BAR S 02 NOT USED

LEGEND

- ** DIMENSION TAKEN NORMAL TO SUBSTRUCTURE UNIT.
- KEYED CONSTRUCTION JOINT FORMED BY BEVELED 2"x6".
- 18" RUBBERIZED MEMBRANE WATERPROOFING, SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.
- C501 OR P511 SPACED 1'-0", COATED. BARS MAY BE PLACED AFTER CONCRETE HAS BEEN POURED BUT PRIOR TO ITS INITIAL SET EMBED 1'-0".



S508

S509

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY KAM		PLANS CK'D. MSK	
SUPERSTRUCTURE DETAILS			SHEET 11 OF 12

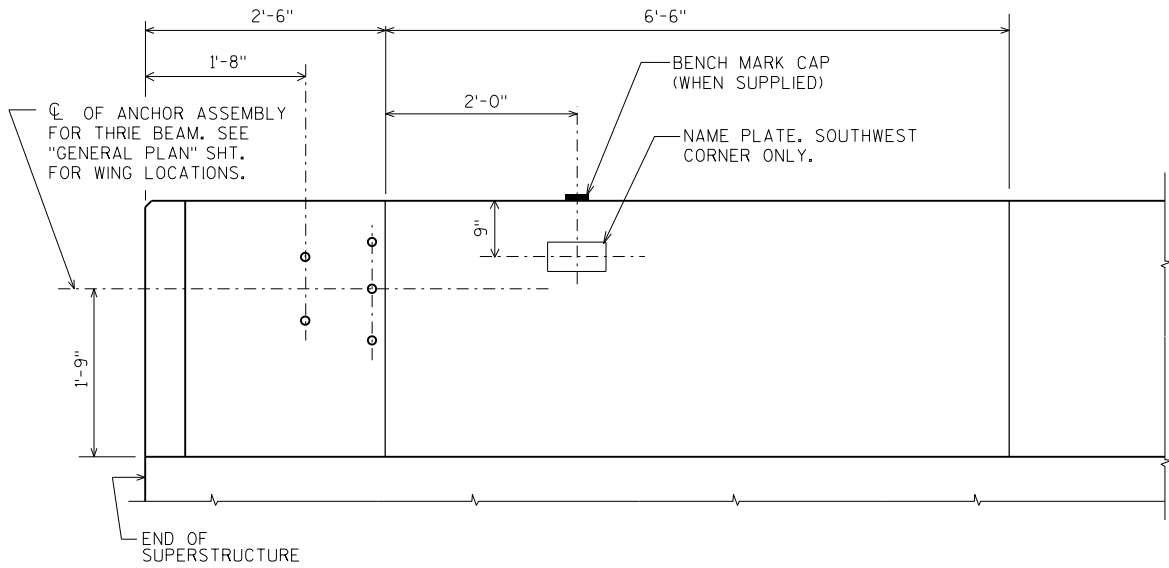
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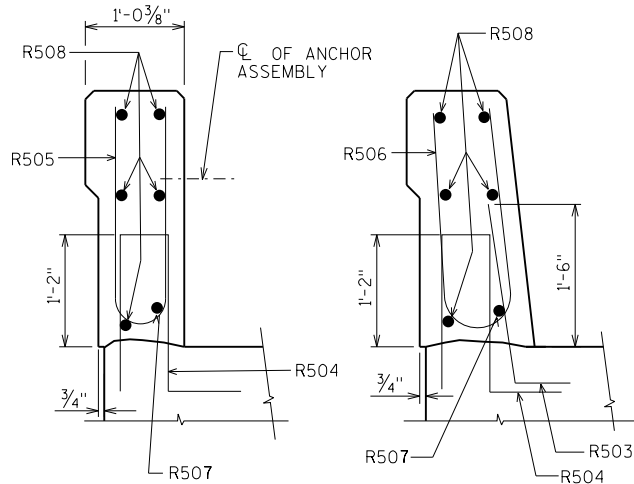
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BATCH PRINT SHEET 12 OF 12

8

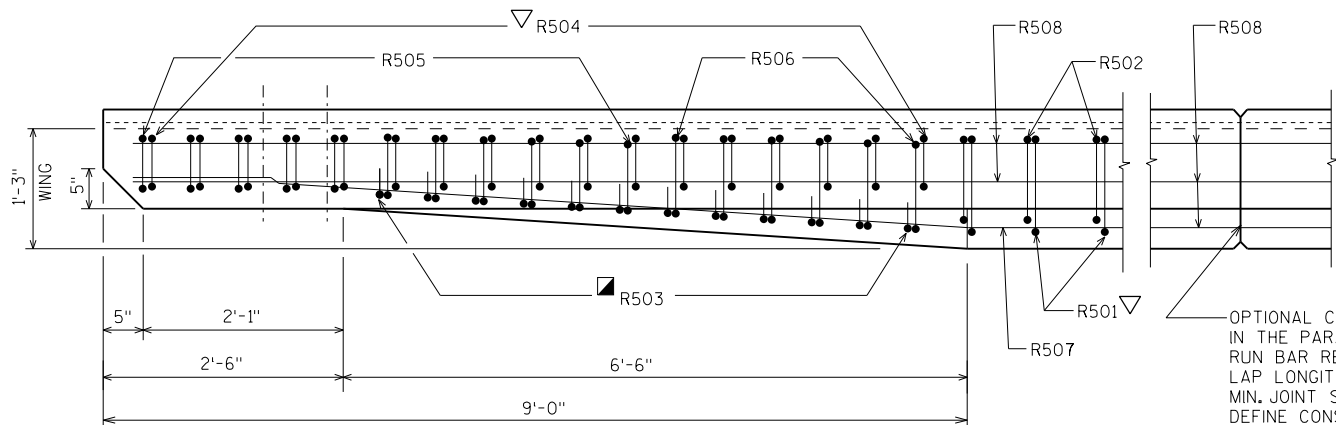


INSIDE ELEVATION



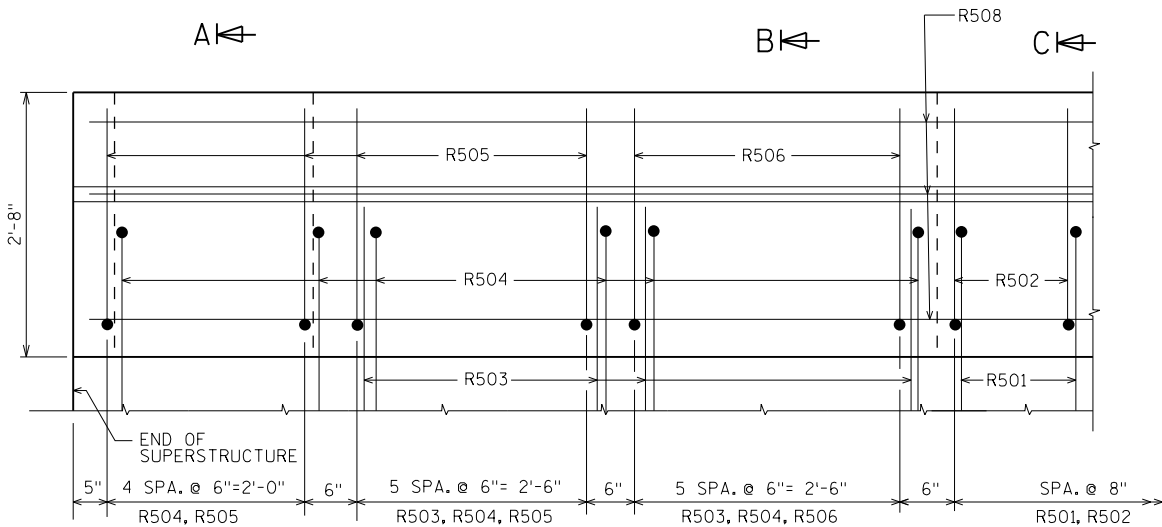
SECTION A

SECTION B

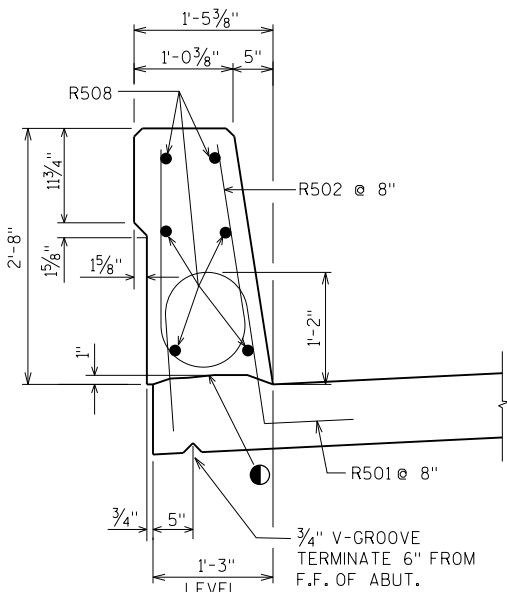


PLAN

(WINGS 1, 2, 3 & 4)



OUTSIDE ELEVATION



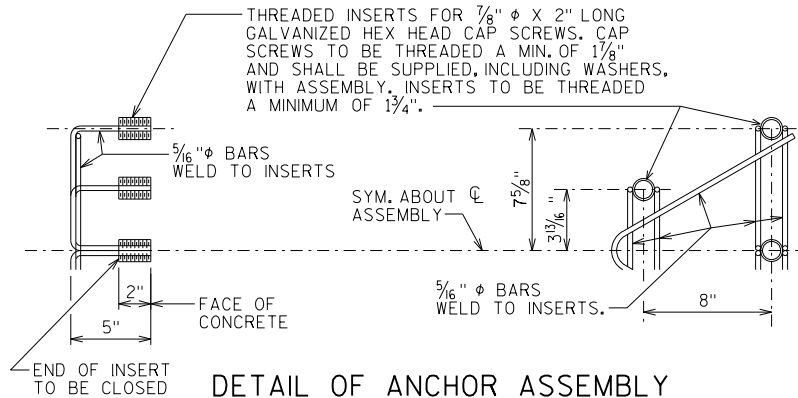
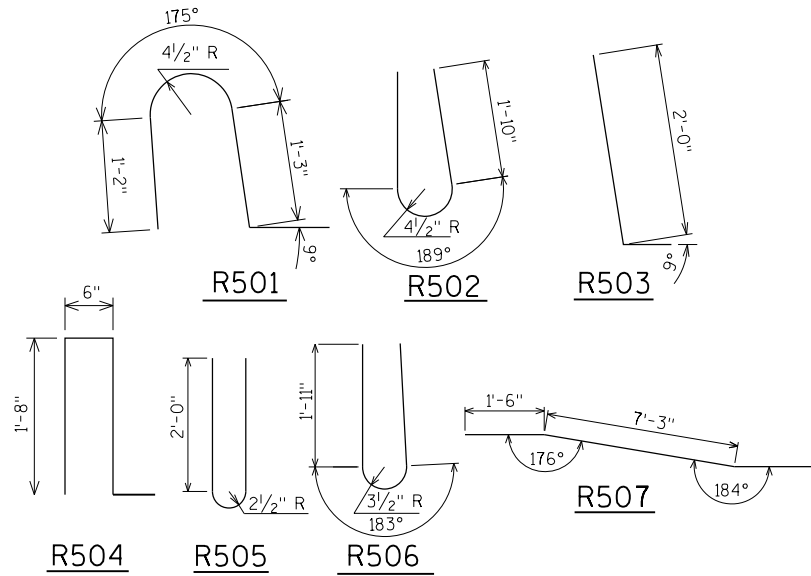
SECTION C

BILL OF BARS

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.

ALL BAR STEEL REINFORCEMENT SHOWN IN THIS BILL SHALL BE EPOXY COATED.

MARK	NO.	REQ'D.	LENGTH	BENT	LOCATION	TOTAL WEIGHT = 3,920 LBS
R501	204	4 - 5		X	PARAPET - BOTTOM	VERT.
R502	204	5 - 0		X	PARAPET - TOP	VERT.
R503	48	2 - 9		X	PARAPET - TAPERED END	VERT.
R504	68	4 - 4		X	PARAPET - TAPERED END	VERT.
R505	44	4 - 9		X	PARAPET - TAPERED END	VERT.
R506	24	4 - 10		X	PARAPET - TAPERED END	VERT.
R507	4	22 - 6		X	PARAPET - TAPERED END	HORIZ.
R508	44	22 - 6			PARAPET	HORIZ.



DETAIL OF ANCHOR ASSEMBLY

NOTE: HEX HEAD CAP SCREWS & WASHERS TO BE GALVANIZED IN ACCORDANCE WITH AASHTO M232 CLASS C.

ASSEMBLY SHALL BE BID ITEM "ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD", EACH.

● CONST. JOINT - STRIKE OFF AS SHOWN.

■ R503 BARS MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. USE CARE TO PLACE R503 OR S503 BARS CORRECTLY ALONG TRANSITION OF PARAPET.

▽ R501 AND R504 BARS TO BE TIED TO SUPERSTRUCTURE STEEL BEFORE SUPERSTRUCTURE IS POURED.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-54-125			
DRAWN BY KAM		PLANS CK'D. MSK	
SINGLE SLOPE PARAPET 32SS			SHEET 12 OF 12

8

CTH V West										
		AREA (SF)			Incremental Vol (CY) (Unadjusted)			Cumulative Vol (CY)		Mass Ordinate (3)
Station	Distance	Cut (1)	Salvaged/ Unusable Pavement Material	Fill	Cut (1)	Salvaged/ Unusable Pavement Material	Fill	Cut (1) 1.00	Fill (2) 1.00	
8+10.00	0	45	0	0	0	0	0	0	0	0
8+50.00	40	43	0	43	65	0	32	65	32	33
9+00.00	50	42	0	52	79	0	88	144	120	24
9+50.00	50	34	0	124	70	0	163	214	284	-69
9+66.61	16.61	30	0	41	19	0	51	234	334	-100
					234	0	334			

CTH V East										
		AREA (SF)			Incremental Vol (CY) (Unadjusted)			Cumulative Vol (CY)		Mass Ordinate (3)
Station	Distance	Cut (1)	Salvaged/ Unusable Pavement Material	Fill	Cut (1)	Salvaged/ Unusable Pavement Material	Fill	Cut (1) 1.00	Fill (2) 1.00	
10+38.55		29	0	38	0	0	0	0	0	0
10+50.00	11.45	29	0	5	12	0	9	12	9	3
11+00.00	50	42	0	0	65	0	4	77	13	64
11+50.00	50	63	0	0	97	0	0	174	13	161
11+95.00	45	63	0	0	105	0	0	279	13	266
12+00.00	5	26	0	0	8	0	0	287	13	274
12+50.00	50	11	0	0	35	0	0	322	13	308
					322	0	13			

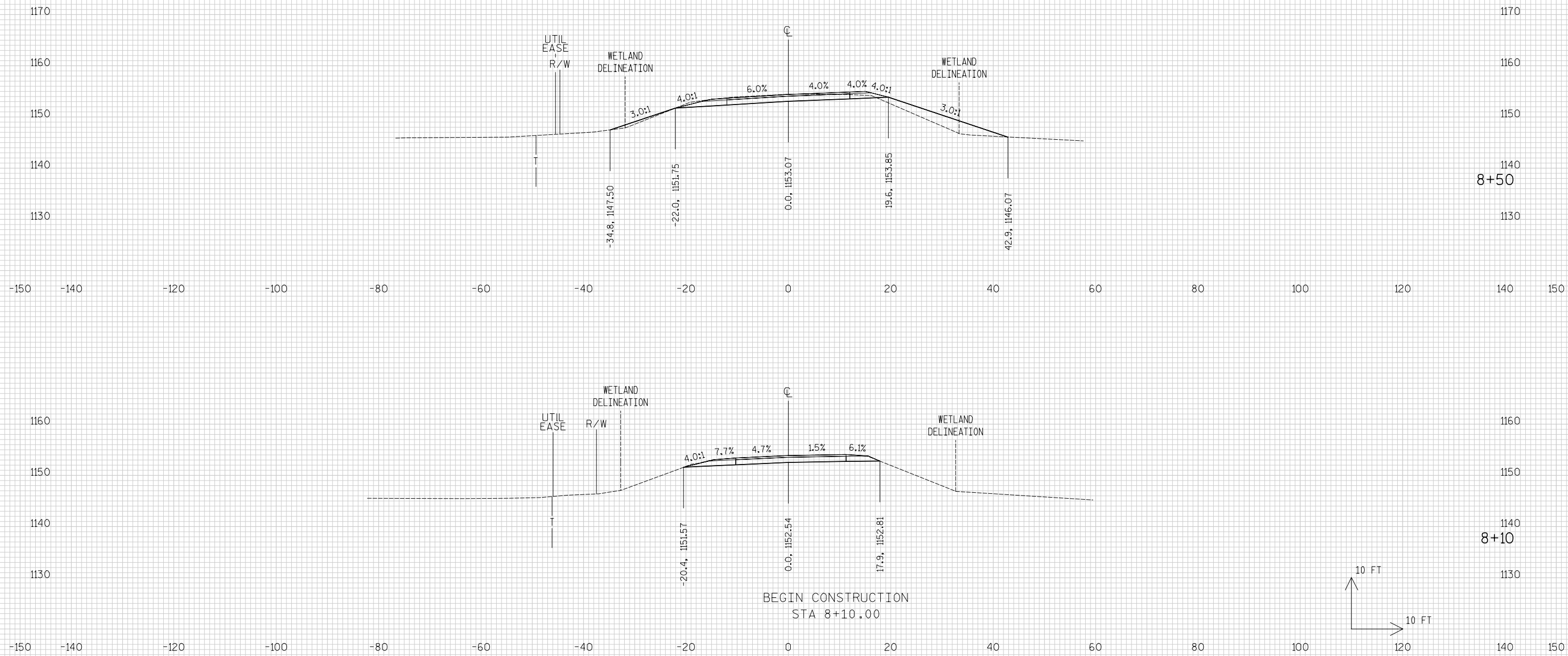
CEMETERY ROAD										
		AREA (SF)			Incremental Vol (CY) (Unadjusted)			Cumulative Vol (CY)		Mass Ordinate (3)
Station	Distance	Cut (1)	Salvaged/ Unusable Pavement Material	Fill	Cut (1)	Salvaged/ Unusable Pavement Material	Fill	Cut (1) 1.00	Fill (2) 1.00	
49+00	0	48	0	0	0	0	0	0	0	0
49+50	50	45	0	4	86	0	3	86	3	83
49+63	13	78	0	1	30	0	1	116	4	112
49+85	22	99	0	0	72	0	0	188	4	184
					188	0	4			

Notes:

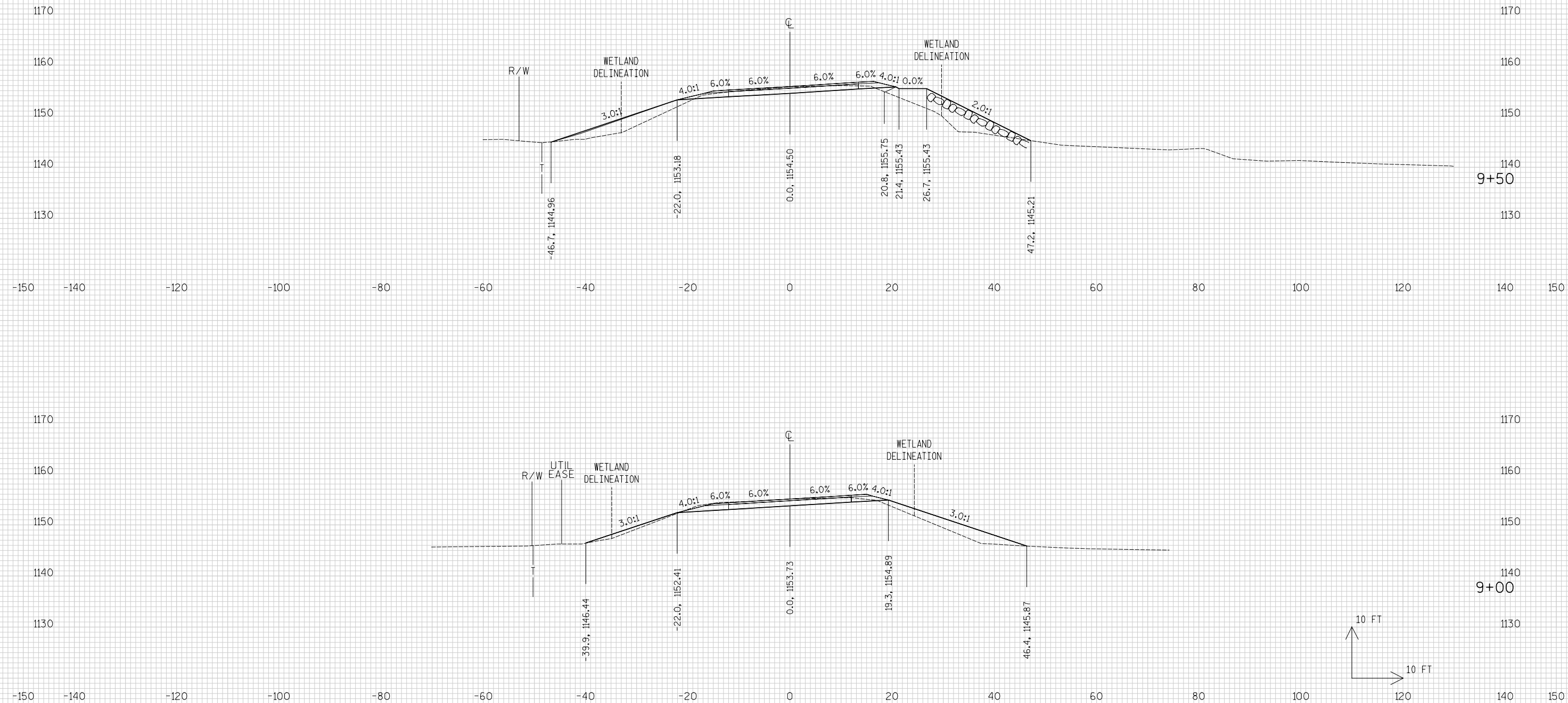
1) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.

2) FILL. FACTOR = 1.0.

3) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION. MASS ORDINATE = AVAILABLE MATERIAL - FILL. THE MASS ORDINATE IS FOR INFORMATION PURPOSES ONLY AS COMMON EXCAVATION AND ROADWAY EMBANKMENT ARE NOT BALANCED FOR QUANTITY PURPOSES AND DOES NOT GUARANTEE THE QUALITY OF COMMON EXCAVATION, AND IF IT CAN BE REUSED ONSITE. ALL EBS EXCAVATION MATERIAL IS ASSUMED TO BE WASTED OFFSITE UNLESS OTHERWISE NOTED ON THE PLANS.

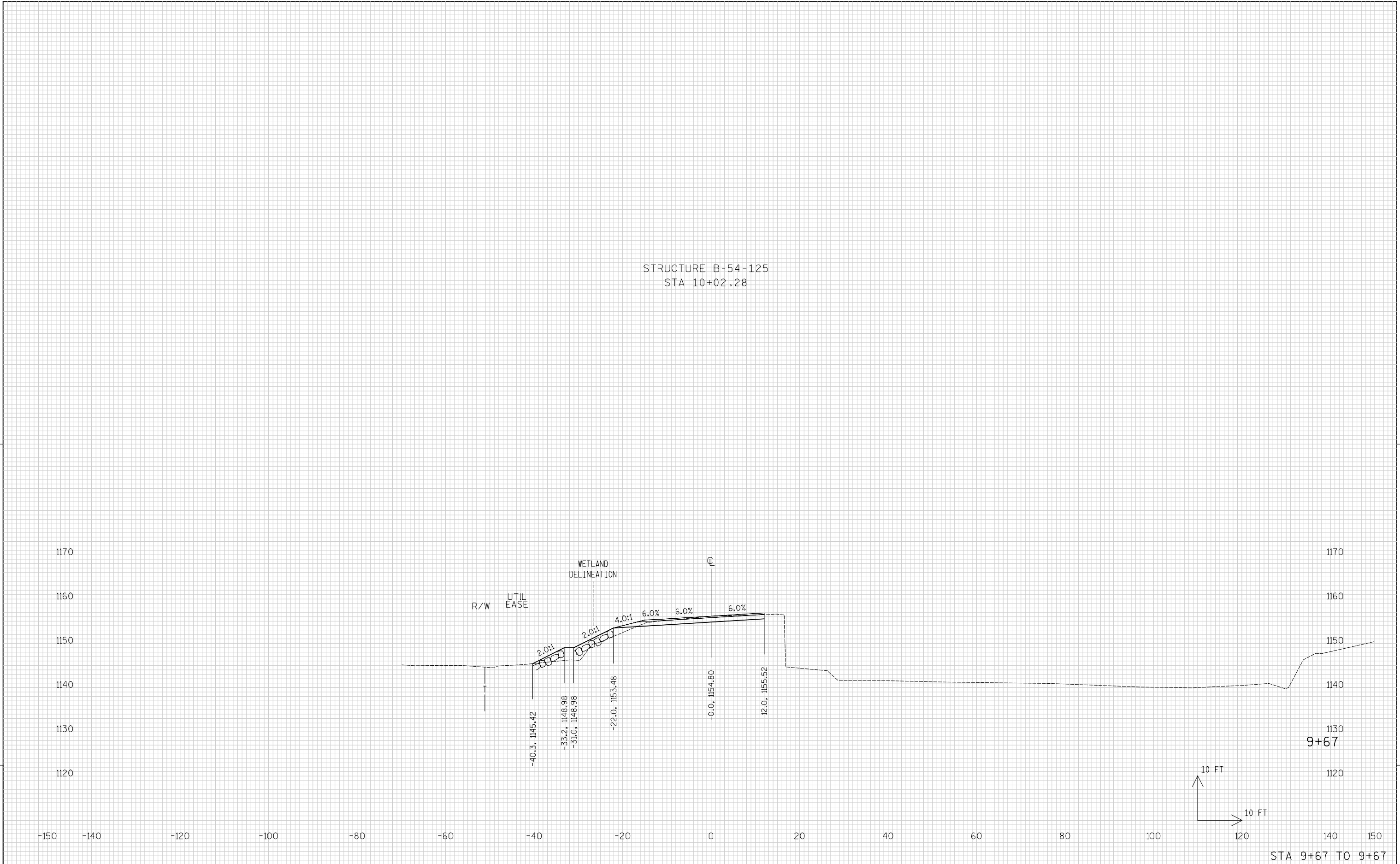


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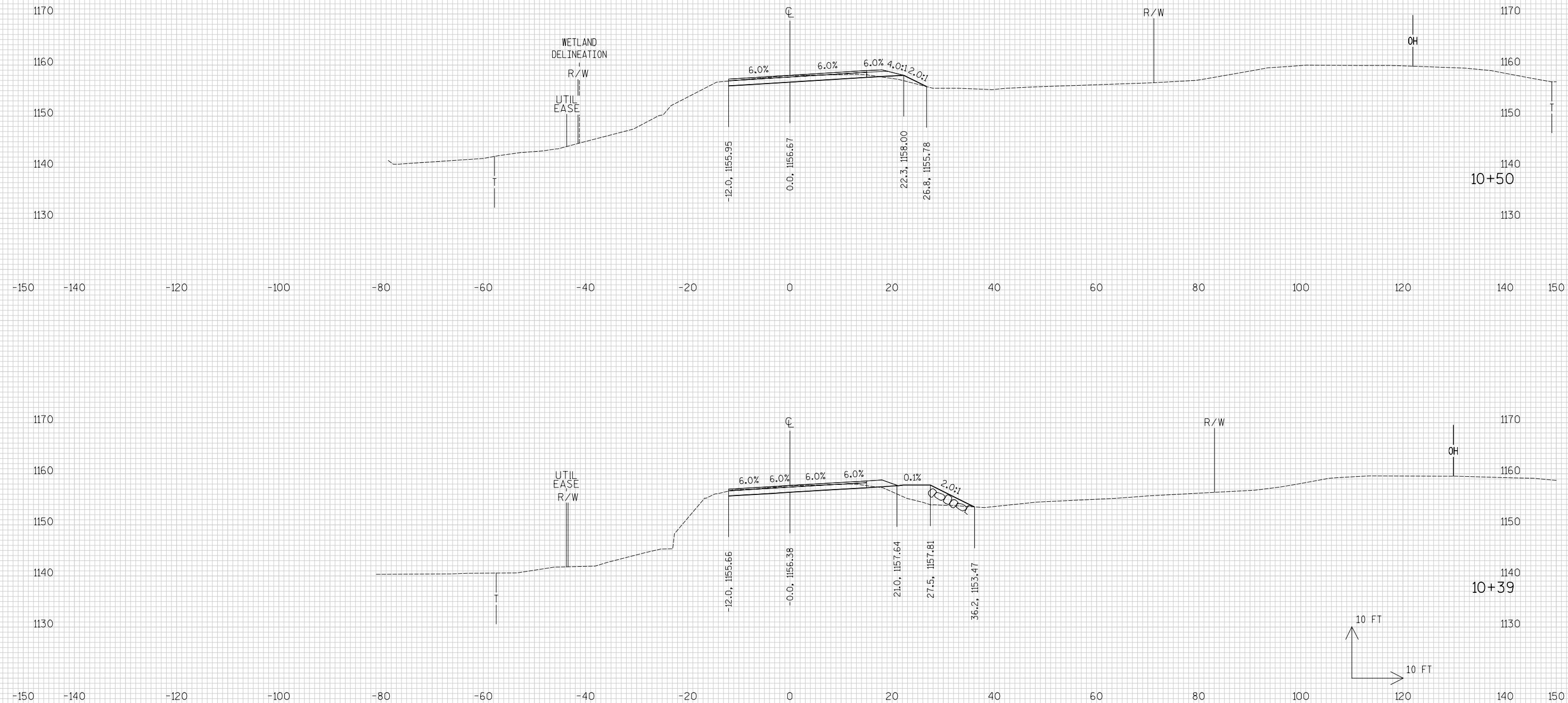


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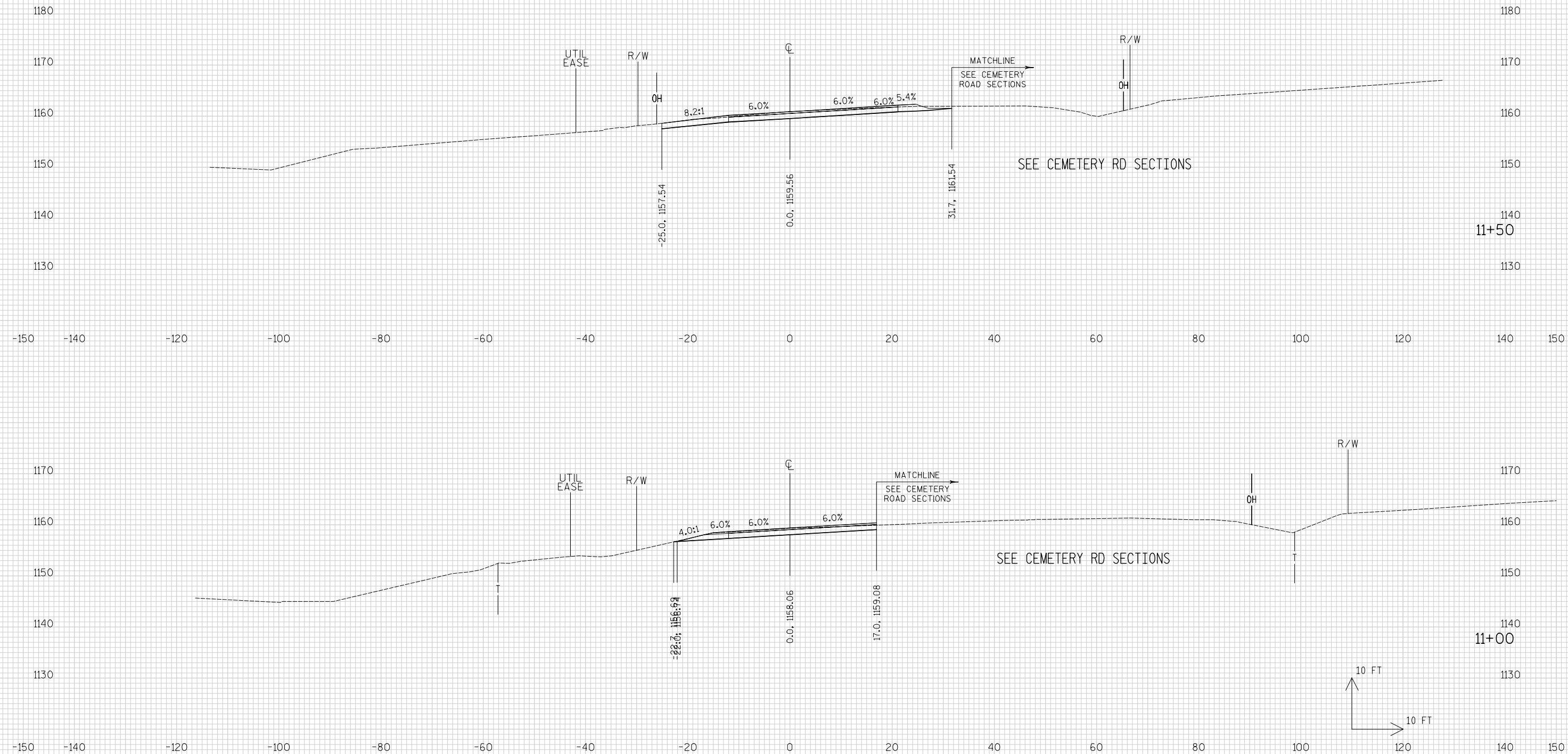
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STATE PROJECT NUMBER:8796-00-70	HWY: CTH V	COUNTY: RUSK	CROSS SECTIONS: CTH V	SHEET	E
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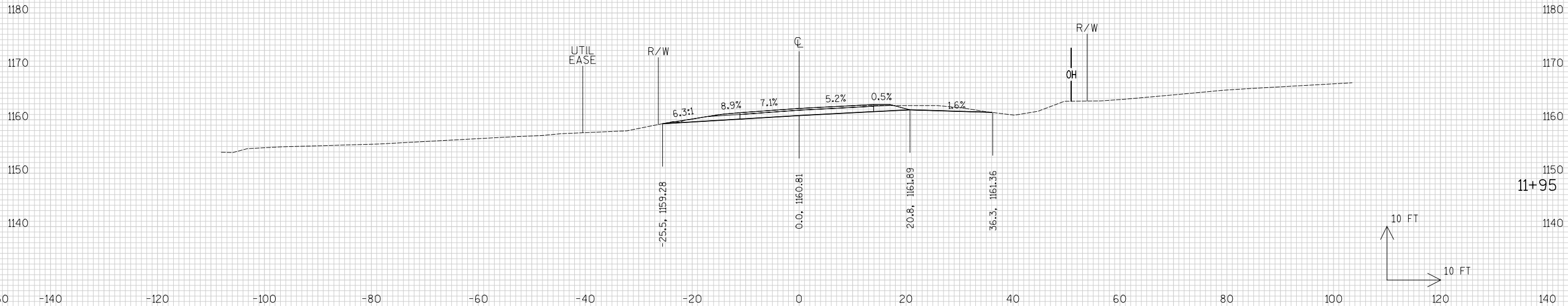


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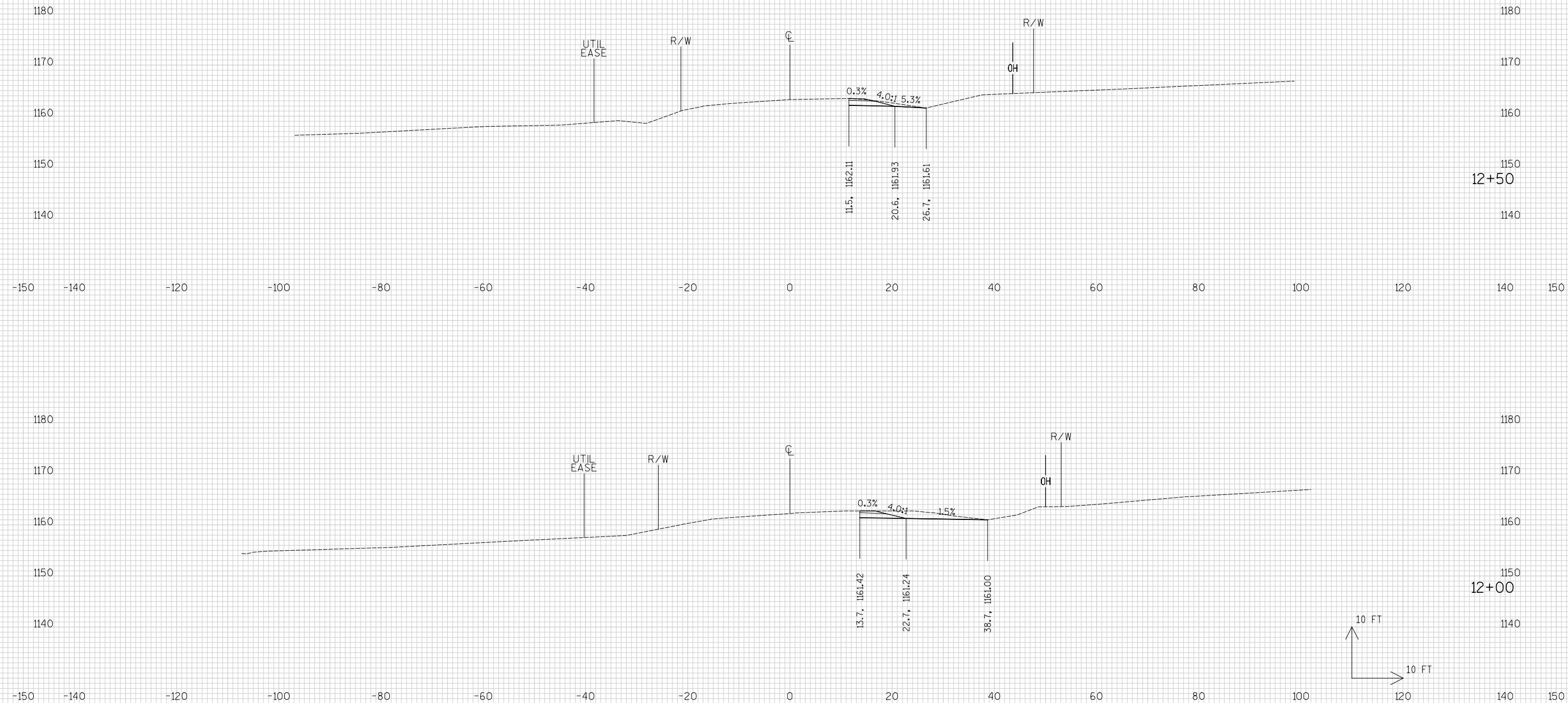
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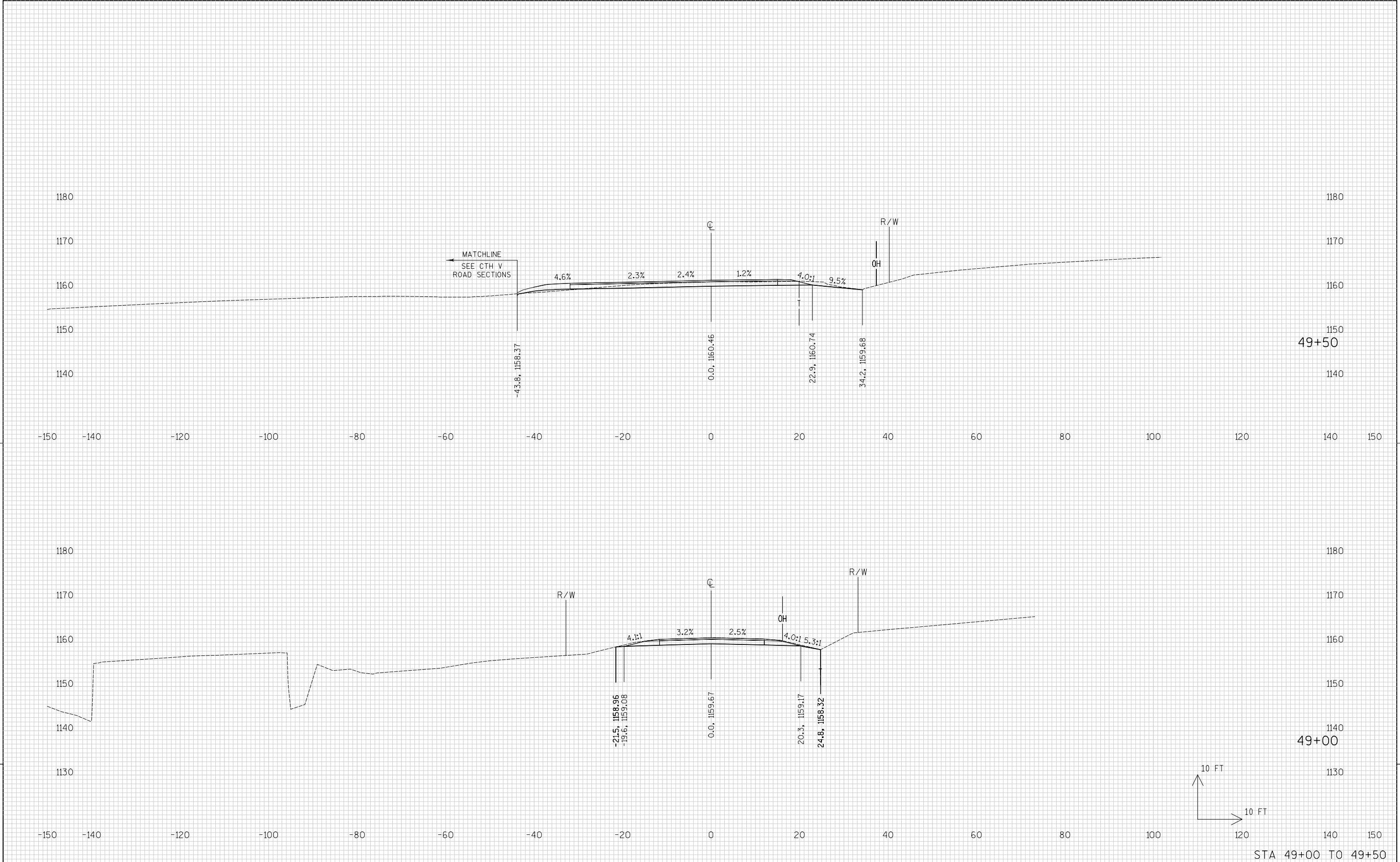
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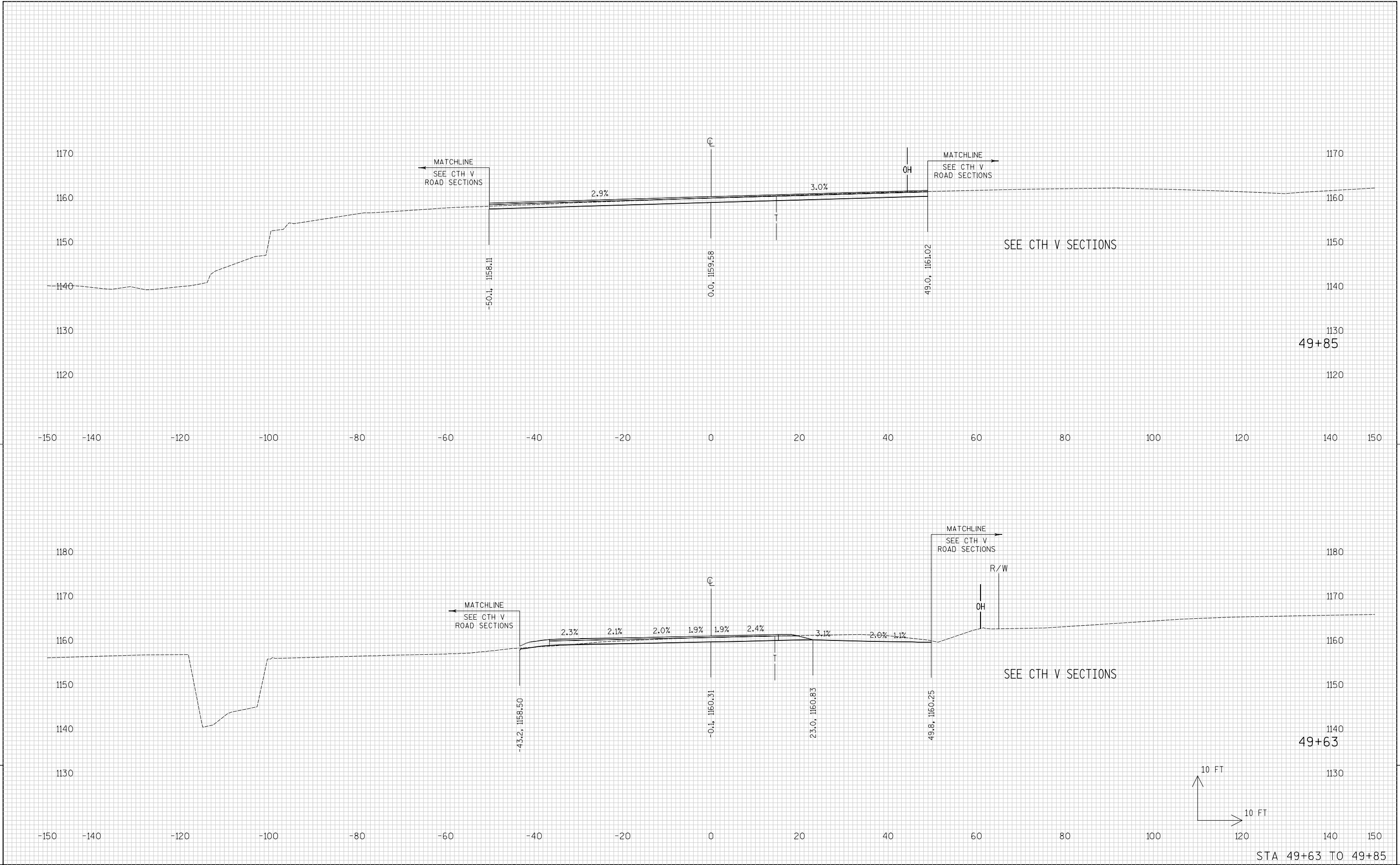
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STATE PROJECT NUMBER:8796-00-70	HWY: CTH V	COUNTY: RUSK	CROSS SECTIONS: CEMETERY ROAD	SHEET	E
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Notes



Wisconsin Department of Transportation

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