RHI JUNE 2017

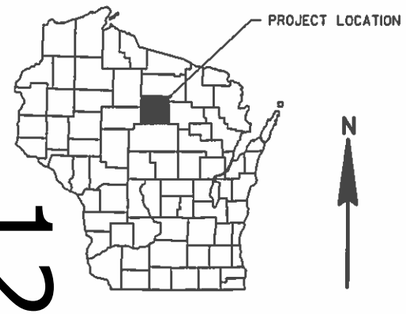
PROJECT ID: 9000-05-60

COUNTY: LINCOLN

ORDER OF SHEETS



TOTAL SHEETS = 42




12

DESIGN DESIGNATION

| | | | |
|---|---|---|---|
| A.A.D.T. | 2017 | = | 680 |
| A.A.D.T. | 2037 | = | 800 |
| D.H.V. | 2037 | = | 150 |
| D.D. | | = | 61/39 |
| T.(DHV) | | = | 19.8% |
| DESIGN SPEED | | = | 60 MPH |
| ESALS | | = | 262,800 |

CONVENTIONAL SYMBOLS

PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES
- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

# STATE OF WISCONSIN
# DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

| STATE PROJECT | FEDERAL PROJECT | |
|---|---|---|
| | PROJECT | CONTRACT |
| 9000-05-60 | ——— | ——— |
| | | |
| | | |
| | | |

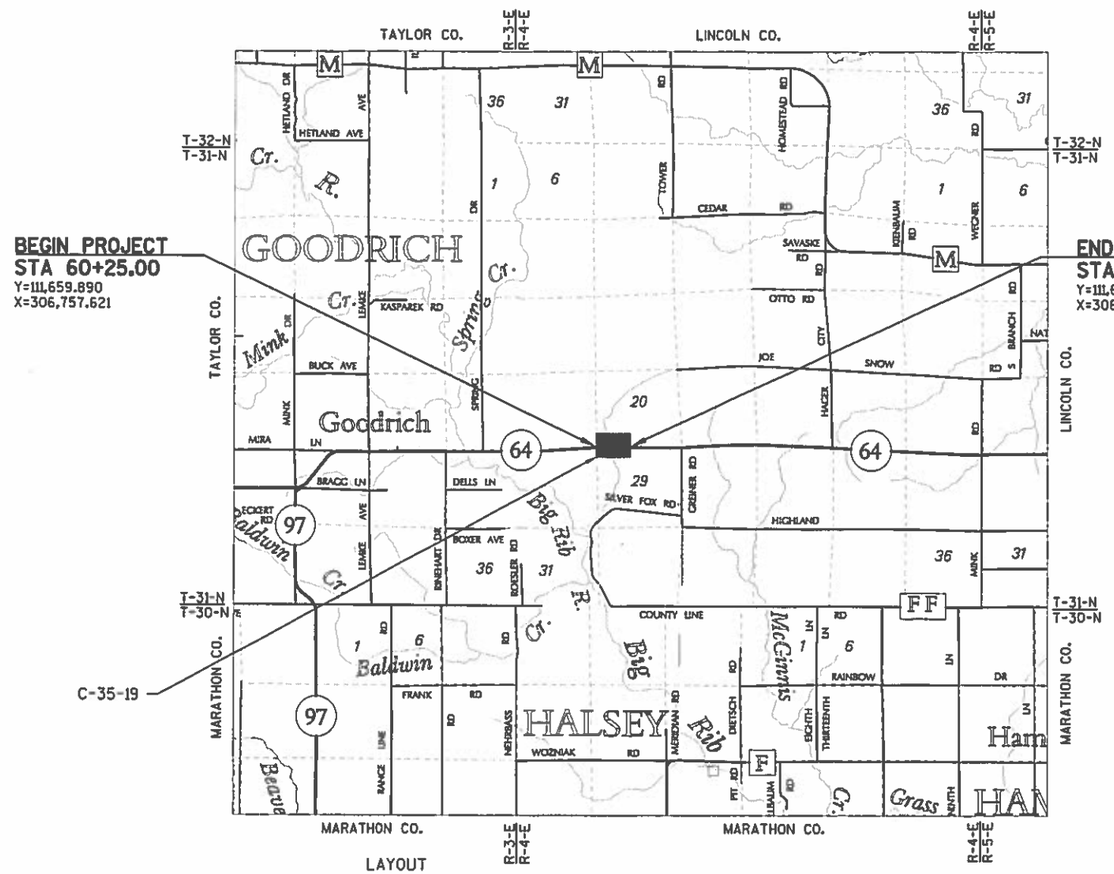
# MEDFORD-MERRILL

**CULVERT REPLACEMENT C-35-19**

## STH 64

LINCOLN COUNTY

| STATE PROJECT NUMBER |
|---|
| 9000-05-60 |




LAYOUT

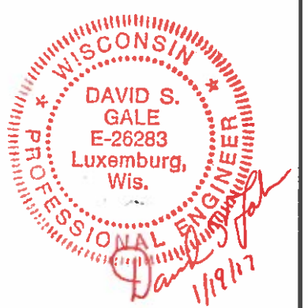
SCALE 0 2 MILE

TOTAL NET LENGTH OF CENTERLINE = 0.038 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE SYSTEM, LINCOLN COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES. VERTICAL DATUM IS NAVD 88.

ORIGINAL PLANS PREPARED BY

OMNNI ASSOCIATES




STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

| | |
|---|---|
| Surveyor | OMNNI ASSOCIATES |
| Designer | OMNNI ASSOCIATES |
| Project Manager | JED P. PETERS |
| Regional Examiner | |
| Regional Supervisor | R. STAFFORD |

APPROVED FOR THE DEPARTMENT

DATE: 1/19/17 (Signature)

E

FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLANS\STH 64\90000560-010101-TI.DWG
90000560-010101-ti

PLOT DATE : 1/19/2017 9:52 AM

PLOT BY : ANDREW WESTBROOK PLOT NAME :

## GENERAL NOTES

LOCATIONS OF EXISTING AND PROPOSED UTILITY FACILITIES AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY ALSO BE OTHER UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

## RUNOFF COEFFICIENT TABLE

| | HYDROLOGIC SOIL GROUP | | | | | | | | | | | |
|---|---|---|---|---|---|---|---|---|---|---|---|---|
| | A | | | B | | | C | | | D | | |
| | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | |
| LAND USE: | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER |
| ROW CROPS | 0.08 | 0.16 | 0.22 | 0.12 | 0.20 | 0.27 | 0.15 | 0.24 | 0.33 | 0.19 | 0.28 | 0.38 |
| | 0.22 | 0.30 | 0.38 | 0.26 | 0.34 | 0.44 | 0.30 | 0.37 | 0.50 | 0.34 | 0.41 | 0.56 |
| MEDIAN STRIP - | 0.19 | 0.20 | 0.24 | 0.19 | 0.22 | 0.26 | 0.20 | 0.23 | 0.30 | 0.20 | 0.25 | 0.30 |
| TURF | 0.24 | 0.26 | 0.30 | 0.25 | 0.28 | 0.33 | 0.26 | 0.30 | 0.37 | 0.27 | 0.32 | 0.40 |
| SIDE SLOPE - | | | 0.25 | | | 0.27 | | | 0.28 | | | 0.30 |
| TURF | | | 0.32 | | | 0.34 | | | 0.36 | | | 0.38 |
| PAVEMENT: | | | | | | | | | | | | |
| ASPHALT | .70 - .95 | | | | | | | | | | | |
| CONCRETE | .80 - .95 | | | | | | | | | | | |
| BRICK | .70 - .80 | | | | | | | | | | | |
| DRIVES, WALKS | .75 - .85 | | | | | | | | | | | |
| ROOFS | .75 - .95 | | | | | | | | | | | |
| GRAVEL ROADS, SHOULDERS | .40 - .60 | | | | | | | | | | | |

TOTAL PROJECT AREA = 0.8 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.5 ACRES

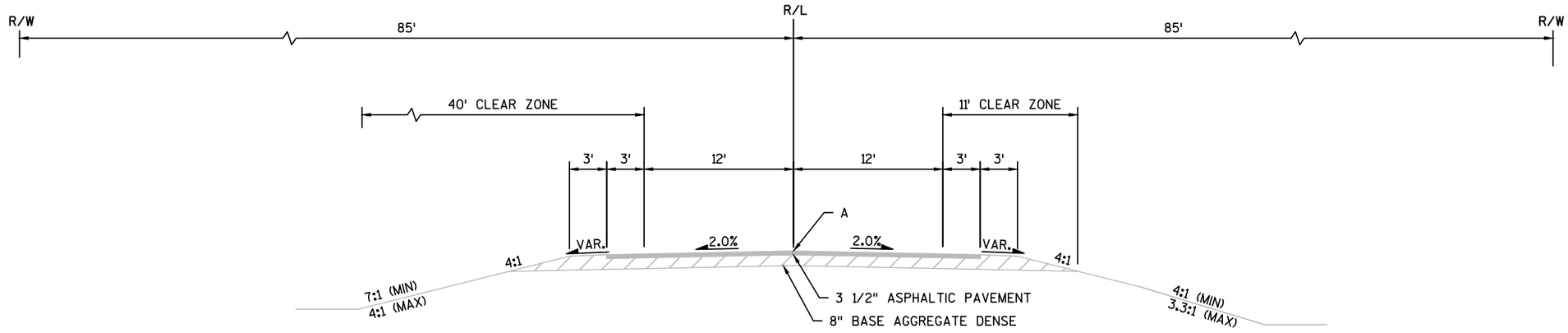
## OTHER CONTACTS

DNR LIAISON

DEPARTMENT OF NATURAL RESOURCES
NORTHERN REGION HEADQUARTERS
107 SUTLIFF
RHINELANDER, WI 54501
ATTN: JON SIMONSEN
TELEPHONE: 715-367-1936
E-MAIL: JONATHAN.SIMONSEN@WISCONSIN.GOV

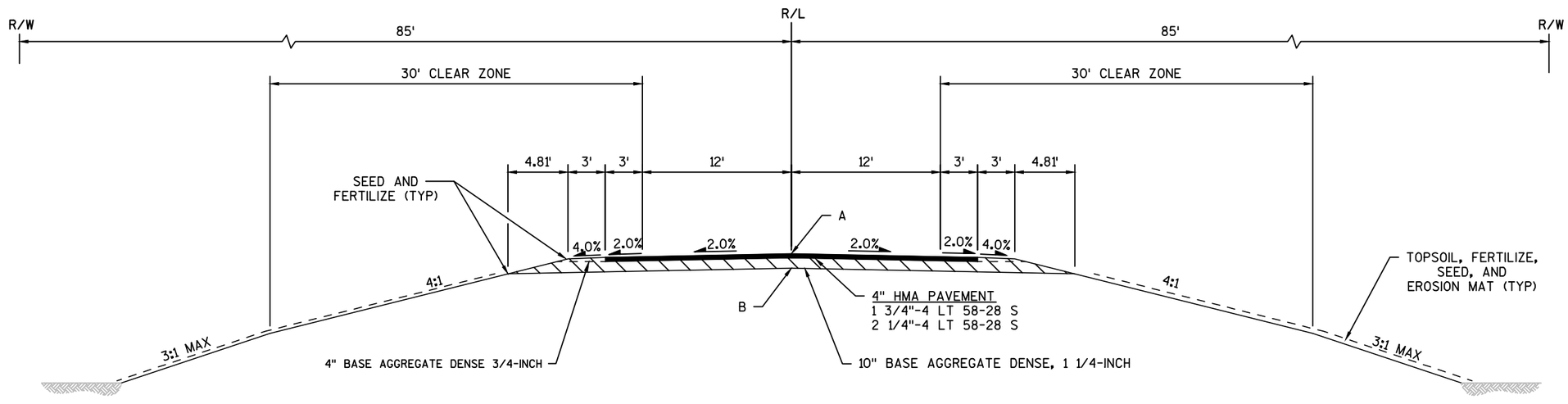
DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

ORDER OF SECTION 2 SHEETS




TYPICAL EXISTING SECTION STH 64

STA 60+25 TO STA 62+25

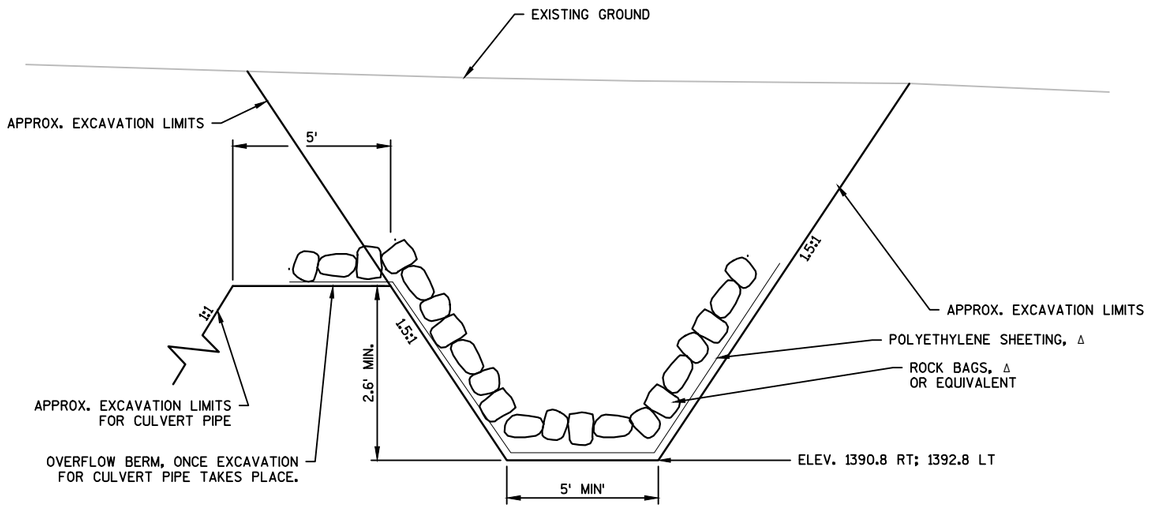



TYPICAL FINISHED SECTION STH 64

STA 60+25 TO STA 62+25

NOTE A; POINT REFERRED TO ON PROFILE.
NOTE B; POINT REFERRED TO ON CROSS SECTIONS.

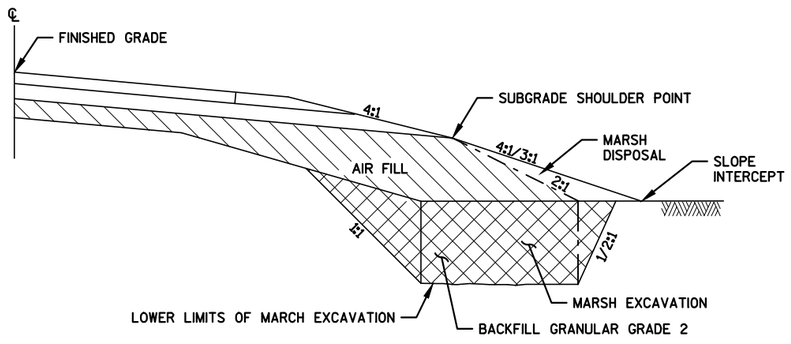
| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | TYPICAL SECTIONS | SHEET 3 | E |
|---|---|---|---|---|---|



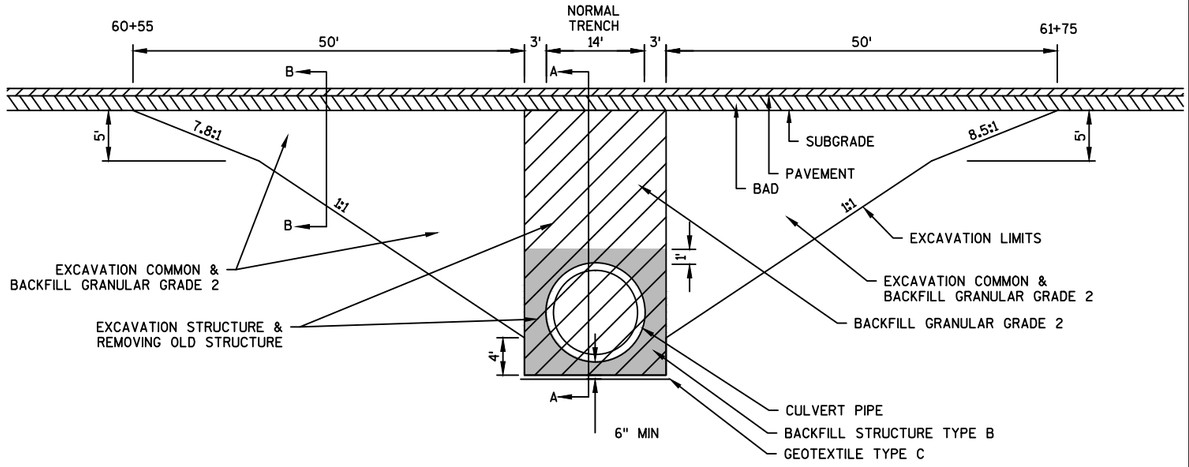

## TEMPORARY WATER DIVERSION CHANNEL

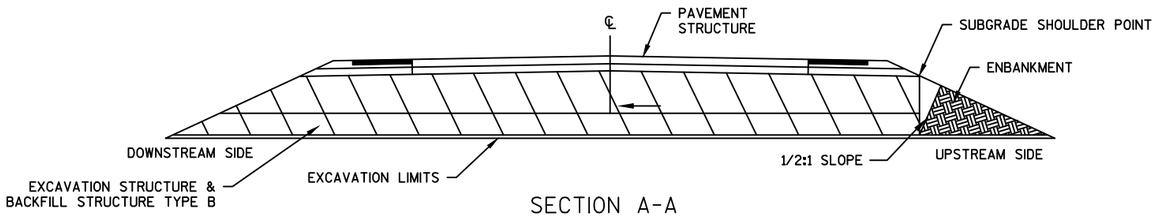
Δ INCIDENTAL TO TEMPORARY WATER DIVERSION

NOTE: EXCEPT FOR MARSH EXCAVATION AND MARSH BACKFILL, EXCAVATION AND BACKFILL FOR THE DIVERSION CHANNEL IS INCLUDED IN THE TEMPORARY WATER DIVERSION ITEM.

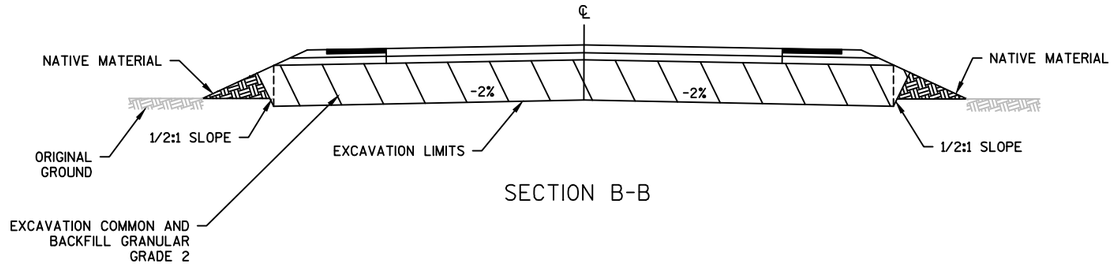



## TYPICAL MARSH EXCAVATION




SECTION A-A




SECTION B-B

## PIPE CULVERT EXCAVATION AND INSTALLATION DETAIL

STA 61+15

2

N

TEMPORARY WATER DIVERSION LOCATION

BRANCH BIG RIB RIVER

REMOVE C-35-1
10'X7'X120' PASP
AND CONCRETE ENDWALLS

EXISTING R/W

WETLAND LIMIT

SLOPE INTERCEPT

PT: 58+96.81

RBS

58+00

60+00

61+00

62+00

63+00

64+00

STH 64

SLOPE INTERCEPT

EXISTING R/W

WETLAND LIMIT

LEGEND

| | |
|---|---|
| ####### | EROSION MAT CLASS I, TYPE B URBAN |
| | SILT FENCE |
| | RIP RAP |
| | SLOPE INTERCEPT |
| | SURFACE WATER FLOW |
| | WETLANDS |
| | PERMITTED WETLAND IMPACTS (PERMANENT) |
| | PERMITTED WETLAND IMPACTS (TEMPORARY) |

$Q_2$ = 37 CFS
ELEV. Q2 = 1394.39
$Q_5$ = 63 CFS
WATER DEPTH IN CHANEL = 2.6'
$V_5$ = 2.9' FPS
SEE TEMPORARY WATER DIVERSION CONSTRUCTION DETAIL
FOR CHANNEL CONFIGURATION

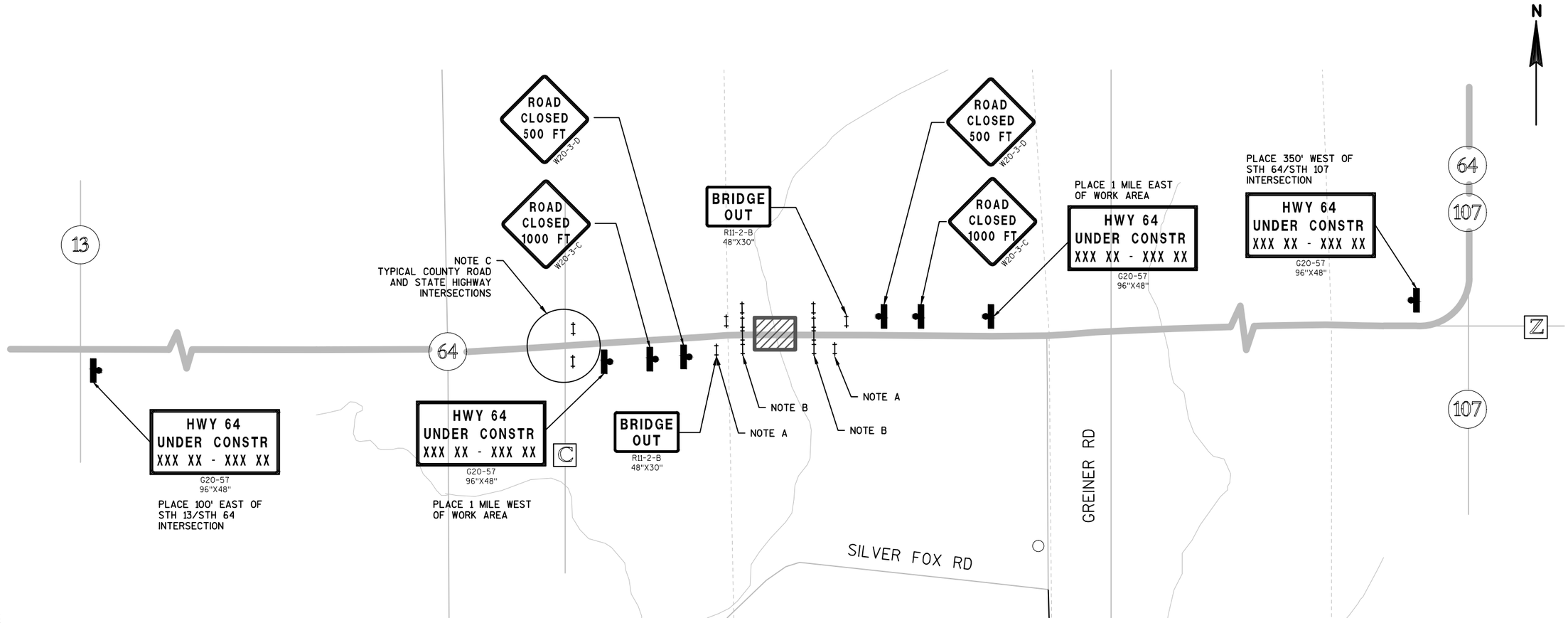
| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | EROSION CONTROL PLAN | SHEET 5 | E |
|---|---|---|---|---|---|

FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLAN\STH 64\90000560-022001-EC.DWG
LAYOUT NAME - 90000560-022001-EC

PLOT DATE : 1/19/2017 4:00 PM PLOT BY : JAIRO MAZARIEGOS PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 42

2




LEGEND

- (00) SIGN NUMBER
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- POST MOUNTED SIGN
- TYPE III BARRICADE
- WORK AREA

NOTE A: USE SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" DETAIL A AND DETAIL E.

NOTE B: USE SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" DETAIL D.

NOTE C: USE SDD "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES "DETAIL 3 SHALL BE USED FOR ALL COUNTY ROAD AND STATE HIGHWAY INTERSECTIONS.

SEE DETOUR PLAN FOR ADDITIONAL SIGNING AND INFORMATION.

TRAFFIC CONTROL GENERAL NOTES

1. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
2. THE SPACING BETWEEN PROPOSED SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.
3. ALL SIGNS SHALL BE 48" x 48" UNLESS OTHERWISE NOTED.
4. "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
5. FIXED MESSAGE SIGNS SHALL BE PLACED ONE WEEK IN ADVANCE OF INITIAL LANE CLOSURE.
6. SEE ADDITIONAL TRAFFIC CONTROL DETAIL SHEETS, DETOUR DETAIL SHEETS AND STANDARD DETAIL DRAWINGS FOR ADDITIONAL TRAFFIC CONTROL REQUIREMENTS.

THIS TRAFFIC CONTROL PLAN IS APPLICABLE DURING THE CLOSURE OF STH 64 AND THE ASSOCIATED DETOUR.

AFTER THE DETOUR IS REMOVED, DAYTIME LANE CLOSURES WITH FLAGGERS WILL BE UTILIZED. SDD "TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)" WILL APPLY TO THE PROJECT. INCLUDE ADDITIONAL ROAD WORK 1/2 MILE SIGN FOR EASTBOUND TRAFFIC.

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | TRAFFIC CONTROL PLAN | SHEET 6 E |
|---|---|---|---|---|

DETOUR ROUTE OVERVIEW

- STH 64 EB; STH 13 SB TO STH 29 EB TO STH 107 NB
- STH 64 WB; STH 107 SB TO STH 29 WB TO STH 13 NB

LEGEND

DETOUR ROUTE

WORK AREA

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | DETOUR PLAN OVERVIEW | SHEET 7 E |
|---|---|---|---|---|

FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLAN\STH 64\90000560-027001-DT.DWG
LAYOUT NAME - 90000560-027001-DT

PLOT DATE : 10/31/2016 2:36 PM PLOT BY : JAIRO MAZARIEGOS PLOT NAME : PLOT SCALE : 1 IN:2 MI

WISDOT/CADDS SHEET 42

201
EAST 64
M3-2 24"X12"
M1-6 24"X24"
DETOUR AHEAD
W20-2-A

202
FROM
MERRILL
ASHLAND
MARSHFIELD
TO
MERRILL
ASHLAND
MARSHFIELD

225
RII-3-B 60"X30"
BRIDGE OUT
13 MILES AHEAD
LOCAL TRAFFIC ONLY
TYPE III BARRICADE

226
M4-9R 30"X24"
DETOUR
TYPE III BARRICADE

221
EAST 64
M3-2 24"X12"
M1-6 24"X24"
DETOUR AHEAD
W20-2-A

222
FROM
MARSHFIELD
MERRILL
CORNELL
TO
MARSHFIELD
MERRILL
CORNELL

223
FROM
EAST 64
SOUTH 13
WEST 64
TO
M4-8 24"X12"
DETOUR
EAST 64
SOUTH 13
WEST 64
M06-1 21"X21"

224
FROM
EAST 64
WEST 64
TO
M4-8 24"X12"
DETOUR
EAST 64
WEST 64
M06-1 21"X21"

24
RII-3-B 60"X30"
BRIDGE OUT
9 MILES AHEAD
LOCAL TRAFFIC ONLY
TYPE III BARRICADE

25
M4-9L 30"X24"
DETOUR
TYPE III BARRICADE

2
FROM
SOUTH 107
WEST 64
TO
DETOUR M4-8 24"X12"
SOUTH 107
WEST 64
M06-1 21"X21"

203
FROM
NORTH 13
EAST 64
SOUTH 13
TO
DETOUR M4-8 24"X12"
NORTH 13
EAST 64
SOUTH 13
M06-1 21"X21"

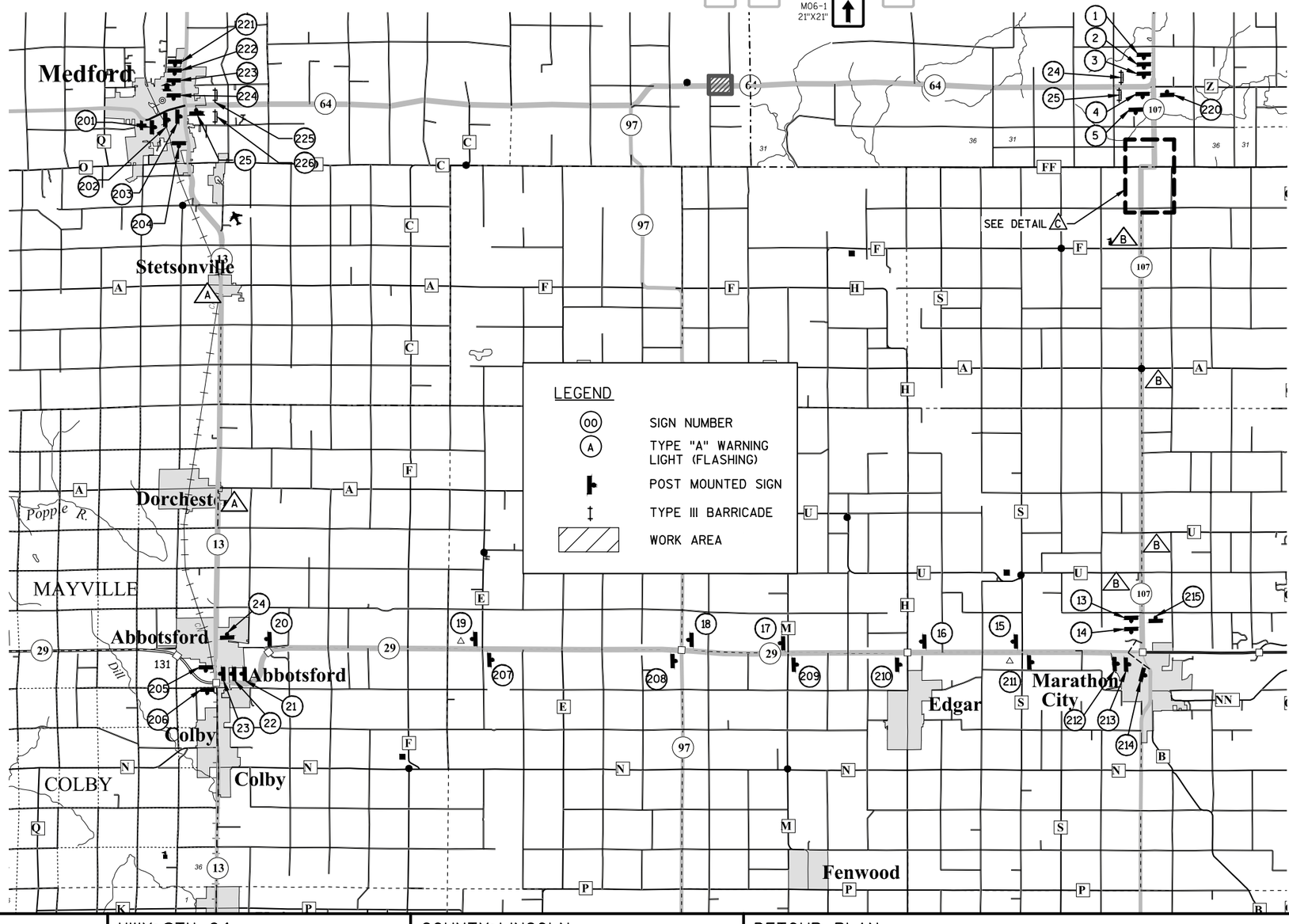
3
FROM
MARATHON CITY
MEDFORD
TO
MARATHON CITY
MEDFORD

N

WEST 64
M3-4 24"X12"
M1-6 24"X24"

1
DETOUR AHEAD
W20-2-A

SEE DETAIL SHEET FOR A - B - C




204
M4-8 24"X12"
DETOUR
M3-2 24"X12"
EAST
M1-6 24"X24"
64
209
207
210
208
211
215

205
M4-8 24"X12"
DETOUR
M3-2 24"X12"
EAST
M1-6 24"X24"
64
M05-1L 21"X21"

206
M4-8 24"X12"
DETOUR
M3-2 24"X12"
EAST
M1-6 24"X24"
64
M06-1 21"X21"

212
M4-8 24"X12"
DETOUR
M3-2 24"X12"
EAST
M1-6 24"X24"
64
M05-2R 21"X21"

213
M4-8 24"X12"
DETOUR
M3-2 24"X12"
EAST
M1-6 24"X24"
64
M06-2 21"X21"

214
M4-8 24"X12"
DETOUR
M3-2 24"X12"
EAST
M1-6 24"X24"
64
M06-1 21"X21"

220
END DETOUR M4-8A 24"X18"
EAST M3-2 24"X12"
64 M1-6 24"X24"

4
FROM
SOUTH 107
WEST 64
TO
DETOUR M4-8 24"X12"
SOUTH 107
WEST 64
M06-1 21"X21"

5
DETOUR M4-8 24"X12"
WEST M3-4 24"X12"
64 M1-6 24"X24"
15 19
16 20
17 24
18

13
M4-8 24"X12"
DETOUR
M3-4 24"X12"
WEST
M1-6 24"X24"
64
M05-1R 21"X21"

14
M4-8 24"X12"
DETOUR
M3-4 24"X12"
WEST
M1-6 24"X24"
64
M06-1 21"X21"

21
M4-8 24"X12"
DETOUR
M3-4 24"X12"
WEST
M1-6 24"X24"
64
M05-2R 21"X21"

22
M4-8 24"X12"
DETOUR
M3-4 24"X12"
WEST
M1-6 24"X24"
64
M06-2 21"X21"

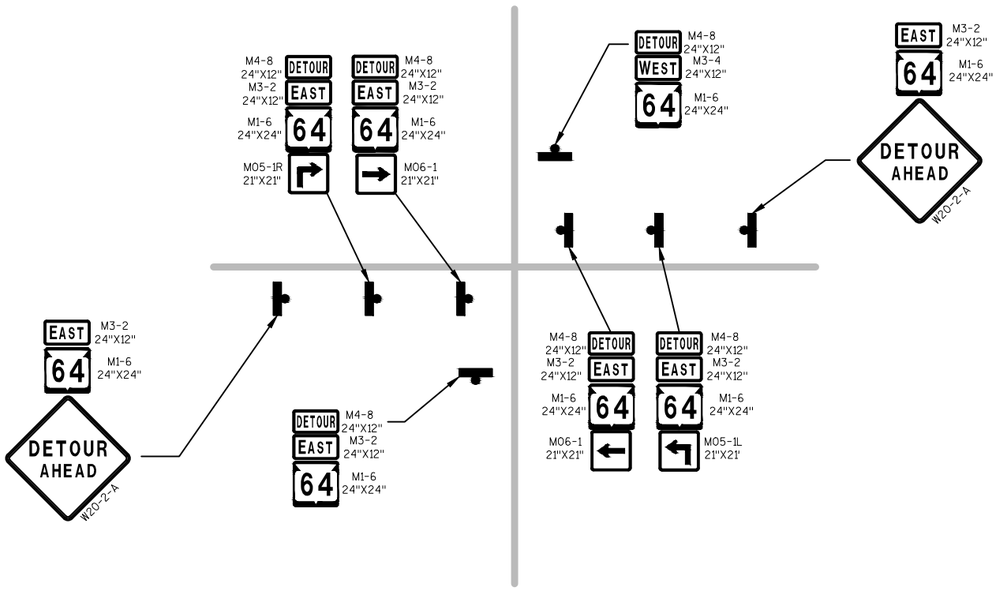
23
M4-8 24"X12"
DETOUR
M3-4 24"X12"
WEST
M1-6 24"X24"
64
M06-1 21"X21"

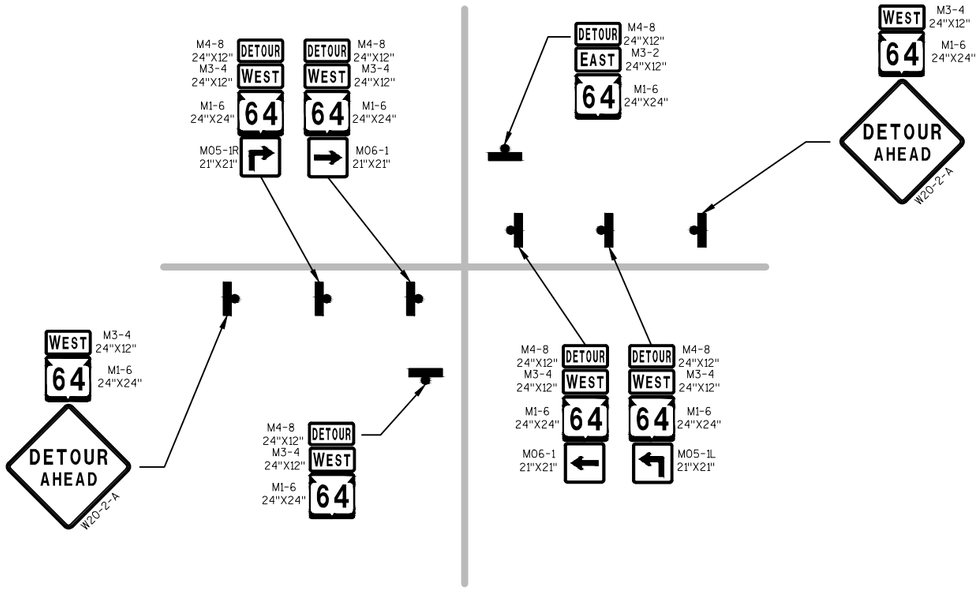
25
END DETOUR M4-8A 24"X18"
WEST M3-4 24"X12"
64 M1-6 24"X24"

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | DETOUR PLAN | SHEET 8 | E |
|---|---|---|---|---|---|

FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLAN\STH 64\90000560-027002-DT.DWG
LAYOUT NAME - 90000560-027001-DT
PLOT DATE : 1/4/2017 3:12 PM
PLOT BY : JAIRO MAZARIEGOS
PLOT NAME :
PLOT SCALE : 1 IN:3 MI
WISDOT/CADDS SHEET 42

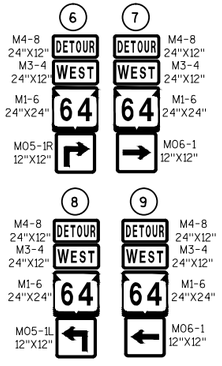
2

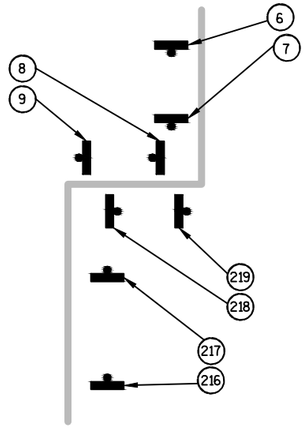



N

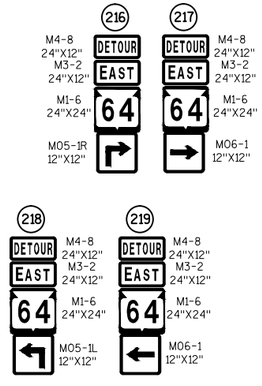
## SIDEROAD TYPICAL A




## DETAIL C

## SIDEROAD TYPICAL B




LEGEND

- DETOUR ROUTE
- (00) SIGN NUMBER
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- POST MOUNTED SIGN
- TYPE III BARRICADE
- WORK AREA

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | DETOUR PLAN DETAIL A - B - C | SHEET 9 | E |
|---|---|---|---|---|---|

9000-05-60

| Line | Item | Item Description | Unit | Total | Qty |
|---|---|---|---|---|---|
| 0010 | 201.0205 | Grubbing | STA | 1.000 | 1.000 |
| 0020 | 203.0200 | Removing Old Structure (station) 01. 61+15 | LS | 1.000 | 1.000 |
| 0030 | 205.0100 | Excavation Common | CY | 1,207.000 | 1,207.000 |
| 0040 | 205.0400 | Excavation Marsh | CY | 106.000 | 106.000 |
| 0050 | 206.2000 | Excavation for Structures Culverts (structure) 01. C-35-19 | LS | 1.000 | 1.000 |
| 0060 | 209.2500 | Backfill Granular Grade 2 | TON | 1,900.000 | 1,900.000 |
| 0070 | 210.2500 | Backfill Structure Type B | TON | 1,040.000 | 1,040.000 |
| 0080 | 213.0100 | Finishing Roadway (project) 01. 9000-05-60 | EACH | 1.000 | 1.000 |
| 0090 | 305.0110 | Base Aggregate Dense 3/4-Inch | TON | 35.000 | 35.000 |
| 0100 | 305.0120 | Base Aggregate Dense 1 1/4-Inch | TON | 550.000 | 550.000 |
| 0110 | 455.0605 | Tack Coat | GAL | 50.000 | 50.000 |
| 0120 | 460.2000 | Incentive Density HMA Pavement | DOL | 100.000 | 100.000 |
| 0130 | 460.5224 | HMA Pavement 4 LT 58-28 S | TON | 155.000 | 155.000 |
| 0140 | 504.0100 | Concrete Masonry Culverts | CY | 77.000 | 77.000 |
| 0150 | 505.0400 | Bar Steel Reinforcement HS Structures | LB | 1,390.000 | 1,390.000 |
| 0160 | 505.0600 | Bar Steel Reinforcement HS Coated Structures | LB | 6,080.000 | 6,080.000 |
| 0170 | 606.0300 | Riprap Heavy | CY | 33.000 | 33.000 |
| 0180 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0190 | 624.0100 | Water | MGAL | 3.000 | 3.000 |
| 0200 | 625.0100 | Topsoil | SY | 2,210.000 | 2,210.000 |
| 0210 | 628.1504 | Silt Fence | LF | 840.000 | 840.000 |
| 0220 | 628.1520 | Silt Fence Maintenance | LF | 840.000 | 840.000 |
| 0230 | 628.1905 | Mobilizations Erosion Control | EACH | 2.000 | 2.000 |
| 0240 | 628.1910 | Mobilizations Emergency Erosion Control | EACH | 2.000 | 2.000 |
| 0250 | 628.2008 | Erosion Mat Urban Class I Type B | SY | 2,210.000 | 2,210.000 |
| 0260 | 629.0210 | Fertilizer Type B | CWT | 1.400 | 1.400 |
| 0270 | 630.0120 | Seeding Mixture No. 20 | LB | 60.000 | 60.000 |
| 0280 | 633.5200 | Markers Culvert End | EACH | 2.000 | 2.000 |
| 0290 | 642.5001 | Field Office Type B | EACH | 1.000 | 1.000 |
| 0300 | 643.0100 | Traffic Control (project) 01. 9000-05-60 | EACH | 1.000 | 1.000 |
| 0310 | 643.0300 | Traffic Control Drums | DAY | 53.000 | 53.000 |
| 0320 | 643.0310.S | Temporary Portable Rumble Strips | LS | 1.000 | 1.000 |
| 0330 | 643.0420 | Traffic Control Barricades Type III | DAY | 343.000 | 343.000 |
| 0340 | 643.0705 | Traffic Control Warning Lights Type A | DAY | 686.000 | 686.000 |
| 0350 | 643.0900 | Traffic Control Signs | DAY | 290.000 | 290.000 |
| 0360 | 643.1000 | Traffic Control Signs Fixed Message | SF | 128.000 | 128.000 |
| 0370 | 643.2000 | Traffic Control Detour (project) 01. 9000-05-60 | EACH | 1.000 | 1.000 |
| 0380 | 643.3000 | Traffic Control Detour Signs | DAY | 4,090.000 | 4,090.000 |
| 0390 | 645.0105 | Geotextile Type C | SY | 90.000 | 90.000 |

9000-05-60

| Line | Item | Item Description | Unit | Total | Qty |
|---|---|---|---|---|---|
| 0400 | 645.0120 | Geotextile Type HR | SY | 70.000 | 70.000 |
| 0410 | 646.0106 | Pavement Marking Epoxy 4-Inch | LF | 650.000 | 650.000 |
| 0420 | 650.4500 | Construction Staking Subgrade | LF | 200.000 | 200.000 |
| 0430 | 650.5000 | Construction Staking Base | LF | 200.000 | 200.000 |
| 0440 | 650.6500 | Construction Staking Structure Layout (structure) 01. C-35-19 | LS | 1.000 | 1.000 |
| 0450 | 650.9910 | Construction Staking Supplemental Control (project) 01. 9000-05-60 | LS | 1.000 | 1.000 |
| 0460 | 650.9920 | Construction Staking Slope Stakes | LF | 200.000 | 200.000 |
| 0470 | 690.0150 | Sawing Asphalt | LF | 60.000 | 60.000 |
| 0480 | 715.0502 | Incentive Strength Concrete Structures | DOL | 396.000 | 396.000 |
| 0490 | SPV.0090 | Special 01. Culvert Pipe Reinforced Concrete 144-Inch Installed | LF | 90.500 | 90.500 |
| 0500 | SPV.0105 | Special 01. Temporary Water Diversion | LS | 1.000 | 1.000 |

3

## EARTHWORK SUMMARY

| STATION | TO | STATION | LOCATION | 205.0100 EXCAVATION COMMON CY NOTE 1 | AVAILIABLE MATERIAL CY NOTE 1 | 205.0400 EXCAVATION MARSH CY | EXPANDED MARSH BACKFILL CY FACTOR 1.50 | UNEXPANDED FILL CY | EXPANDED FILL CY FACTOR 1.25 | MASS ORDINATE CY NOTE 2 | WASTE CY |
|---|---|---|---|---|---|---|---|---|---|---|---|
| CATEGORY 0010 | | | | | | | | | | | |
| 60+25 | - | 62+25 | STH 64 | 347 | 347 | 106 | 159 | 222 | 277 | 70 | 70 |
| 60+55 | - | 61+75 | CULVERT | 860 | 860 | 0 | 0 | 0 | 0 | 860 | 860 |
| | | | PROJECT TOTALS | 1,207 | 1,207 | 106 | 159 | 222 | 277 | 930 | 930 |

NOTES: 1) ALL EXCAVATED ASPHALT MATERIAL ASSUMED USABLE AS FILL.
2) MASS ORDINATE = EXCAVATION COMMON - EXPANDED FILL

## GRUBBING

| STATION TO STATION | | | ROADWAY | 201.0205 GRUBBING STA |
|---|---|---|---|---|
| CATEGORY 0010 | | | | |
| 60+75 | - | 61+75 | STH 64 | 1 |
| | | PROJECT TOTALS | | 1 |

3

## BACKFILL GRANULAR GRADE 2

| STATION TO STATION | | | LOCATION | 209.2500 BACKFILL GRANULAR GRADE 2 TON |
|---|---|---|---|---|
| CATEGORY 0010 | | | | |
| 60+55 | - | 61+75 | STH 64 | 1740 |
| 60+90 | - | 61+60 | WETLANDS | 160 |
| | | PROJECT TOTALS | | 1,900 |

## BASE AGGREGATE DENSE AND WATER

| STATION TO STATION | | | ROADWAY | 305.0110 3/4-INCH TON | 305.0120 1 1/4-INCH TON | 624.0100 WATER MGAL |
|---|---|---|---|---|---|---|
| CATEGORY 0010 | | | | | | |
| 60+25 | - | 62+25 | STH 64 | 33 | 544 | 3 |
| UNDISTRIBUTED | | | STH 64 | 2 | 6 | --- |
| | | PROJECT TOTALS | | 35 | 550 | 3 |

## ASPHALTIC ITEMS

| STATION TO STATION | | | ROADWAY | 455.0605 TACK COAT GAL | 460.5224 HMA PAVEMENT 4 LT 58-28 S TON |
|---|---|---|---|---|---|
| CATEGORY 0010 | | | | | |
| 60+25 | - | 62+25 | STH 64 | 50 | 155 |
| | | PROJECT TOTALS | | 50 | 155 |

## EROSION CONTROL AND RESTORATION

| STATION TO STATION | | | ROADWAY | 625.0100 TOPSOIL SY | 628.1504 SILT FENCE LF | 628.1520 SILT FENCE MAINTENANCE LF | 628.1905 MOBILIZATIONS EROSION CONTROL EA | 628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EA | 628.2008 EROSION MAT URBAN CLASS 1 TYPE B SY | 629.0210 FERTILIZER TYPE B CWT | 630.0120 SEEDING MIXTURE NO. 20 LB |
|---|---|---|---|---|---|---|---|---|---|---|---|
| CATEGORY 0010 | | | | | | | | | | | |
| 60+25 | - | 62+25 | STH 64 | 1,770 | 675 | 675 | 2 | --- | 1,770 | 1.1 | 48 |
| UNDISTRIBUTED | | | STH 64 | 440 | 165 | 165 | --- | 2 | 440 | 0.3 | 12 |
| | | PROJECT TOTALS | | 2,210 | 840 | 840 | 2 | 2 | 2,210 | 1.4 | 60 |

## SAWING

| STATION TO STATION | | | ROADWAY | 690.0150 ASPHALT LF |
|---|---|---|---|---|
| CATEGORY 0010 | | | | |
| 60+25 | - | 62+25 | STH 64 | 60 |
| | | PROJECT TOTALS | | 60 |

## CONSTRUCTION STAKING

| STA | TO | STA | LOCATION | 650.4500 SUBGRADE LF | 650.5000 BASE LF | CATEGORY 0020 650.6500 STRUCTURE LAYOUT LS | 650.9910 SUPPLEMENTAL CONTROL LS | 650.9920 SLOPE STAKES LF |
|---|---|---|---|---|---|---|---|---|
| CATEGORY 0010 | | | | | | | | |
| 60+25 | - | 62+25 | STH 64 | 200 | 200 | --- | --- | 200 |
| STRUCTURE C-35-19 | | | STH 64 | --- | --- | 1 | --- | --- |
| PROJECT | | | STH 64 | --- | --- | --- | 1 | --- |
| | | | PROJECT TOTALS | 200 | 200 | 1 | 1 | 200 |

## PAVEMENT MARKING

| STATION TO STATION | | | ROADWAY | 646.0106 EPOXY 4-INCH WHITE LF | 646.0106 EPOXY 4-INCH YELLOW LF |
|---|---|---|---|---|---|
| CATEGORY 0010 | | | | | |
| 60+25 | - | 62+25 | STH 64 | 400 | 250 |
| | | SUB TOTAL | | 400 | 250 |
| | | PROJECT TOTALS | | 650 | |

## TRAFFIC CONTROL

| PHASE | LOCATION | APPROX. SERVICE PERIOD 13 DAYS | 643.0300 TRAFFIC CONTROL DRUMS NO. | 643.0300 TRAFFIC CONTROL DRUMS DAYS | 643.0310.S TEMPORARY PORTABLE RUMBLE STRIPS LS | 643.0420 BARRICADES TYPE III NO. | 643.0420 BARRICADES TYPE III DAYS | 643.0705 WARNING LIGHTS TYPE A NO. | 643.0705 WARNING LIGHTS TYPE A DAYS | 643.0900 SIGNS NO. | 643.0900 SIGNS DAYS | 643.1000 SIGNS FIXED MESSAGE** NO. | 643.1000 SIGNS FIXED MESSAGE** SF | 643.3000 DETOUR SIGNS NO. | 643.3000 DETOUR SIGNS DAYS |
|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|
| CATEGORY 0010 | | | | | | | | | | | | | | | |
| 1 (DETOUR) | STH 64 - 1 MILE WEST OF PROJECT | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | 1 | 32 | -- | -- |
| | STH 64 - 1 MILE EAST OF PROJECT | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | 1 | 32 | -- | -- |
| | STH 64 AT STH 13 | 13 | -- | -- | -- | 2 | 26 | 4 | 52 | -- | -- | 1 | 32 | 8 | 104 |
| | STH 13 AT STH 64 | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 11 | 143 |
| | STH 64 AT STH 107 | 13 | -- | -- | -- | 2 | 26 | 4 | 52 | -- | -- | 1 | 32 | 2 | 26 |
| | STH 64 AT PROJECT LOCATION | 13 | -- | -- | -- | 14 | 182 | 28 | 364 | 8 | 104 | -- | -- | -- | -- |
| | STH 107 AT STH 64 | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 14 | 182 |
| | STH 107 AT CTH FF | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 32 | 416 |
| | STH 107 AT STH 29 | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 23 | 299 |
| | STH 29 AT STH 13 | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 26 | 338 |
| | STH 64 SIDEROADS | 13 | -- | -- | -- | 6 | 78 | 12 | 156 | 12 | 156 | -- | -- | -- | -- |
| | STH 107 SIDEROADS | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 84 | 1092 |
| | STH 29 SIDEROADS | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 30 | 390 |
| | STH 13 SIDEROADS | 13 | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | -- | 56 | 728 |
| 2 (NO DETOUR) | STH 64 | 3 | 16 | 48 | 1 | -- | -- | -- | -- | 1 | 3 | -- | -- | -- | -- |
| | UNDISTRIBUTED | | | 5 | 0 | | 31 | | 62 | | 27 | | | | 372 |
| | **TOTALS** | | **53** | | **1** | | **343** | | **686** | | **290** | | **128** | | **4,090** |

** FIXED MESSAGE SIGNS TO BE PLACED 1 WEEK PRIOR TO CONSTRUCTION

TREE REMOVALS (BY OTHERS)

BENCHMARK TABLE

| BM# | ELEVATION | DESCRIPTION |
|---|---|---|
| 1 | 1424.58 | SPK IN 8" POPLAR AT STA 56+60 NEAR NORTH R/W LINE |
| 2 | 1420.73 | SPK IN 6" MAPLE AT STA 57+40 NEAR SOUTH R/W LINE |
| 3 | 1398.96 | SPK IN 12" SPRUCE AT STA 60+40 NEAR SOUTH R/W LINE |

BRANCH BIG RIB RIVER

TEMPORARY WATER DIVERSION
SEE CROSS SECTIONS AND CONSTRUCTION DETAILS

EXISTING R/W

N

WETLAND LIMIT

C-35-1
REMOVE 10'X7'X120' PASP
AND ENDWALLS

REMOVE 7'x10'x120' PASP

STA 61+15 C-35-19
CULVERT PIPE REINFORCED CONCRETE
144-INCH (90.5 LF)

SLOPE INTERCEPT

SAWCUT AND
MATCH EXIST. (TYP)

PT: 58+96.81

PROPOSED
PAVEMENT LIMITS

CP 906
Y=111672.948
X=306651.072

59+00 S89° 27' 09"E 60+00 61+00 62+00 63+00 S89° 27' 09"E 64+00

STH 64

CONSTRUCTION REFERENCE LINE

CP 907
Y=111645.259
X=306844.433

PROPOSED STRUCTURE

END PROJECT
STA 62+25.00

SLOPE INTERCEPT

BEGIN PROJECT
STA 60+25.00

WETLAND LIMIT

EXISTING R/W

WOODS

RBS

VPC ELEV = 1410.41
VPC STA = 60+35.00

VPI STA = 61+25.00
VPI ELEV = 1406.36
g1 = -4.50%
g2 = -1.17%
L = 180.00
K = 54

VPT STA = 62+15.00
VPC ELEV = 1405.31
PVI STA = 62+25.00
PVI ELEV = 1405.19

-4.50%

-1.17%

PVI STA = 60+25.00
PVI ELEV = 1410.86

EXISTING R/L PROFILE

PROPOSED R/L PROFILE

REMOVE STRUCTURE C-35-1
STA 61+15
10'x7'x120' PASP
AND CONCRETE ENDWALLS

STRUCTURE C-35-19
STA 61+15
CULVERT PIPE REINFORCED CONCRETE
144-INCH
90.5 LF
INV LT=1388.79
INV RT=1388.33
SKEW 0°

HW 100 ELEV. = 1395.86

OBSERVED WATER ELEV. =1391.8

1420 1415 1410 1405 1400 1395 1390

1420.2 1418.7 1417.3 1415.9 1414.6 1413.3 1412.1 1410.9 1410.86 1409.8 1409.75 1408.8 1408.76 1407.9 1407.87 1407.1 1407.11 1406.6 1406.46 1406.1 1405.92 1405.7 1405.50 1405.2 1405.19 1404.9 1404.6 1404.4 1404.2 1404.2 1404.2 1404.3 1404.5 1404.7

58+50 58+75 59+00 59+25 59+50 59+75 60+00 60+25 60+50 60+75 61+00 61+25 61+50 61+75 62+00 62+25 62+50 62+75 63+00 63+25 63+50 63+75 64+00 64+25 64+50

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | PLAN AND PROFILE: STH 64 | SHEET 14 | E |
|---|---|---|---|---|---|

FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLAN\STH 64\90000560-050101-PP.DWG
LAYOUT NAME - 90000560-050101-PP

PLOT DATE : 2/1/2017 3:21 PM PLOT BY : MATT BERG PLOT NAME : PLOT SCALE : **********

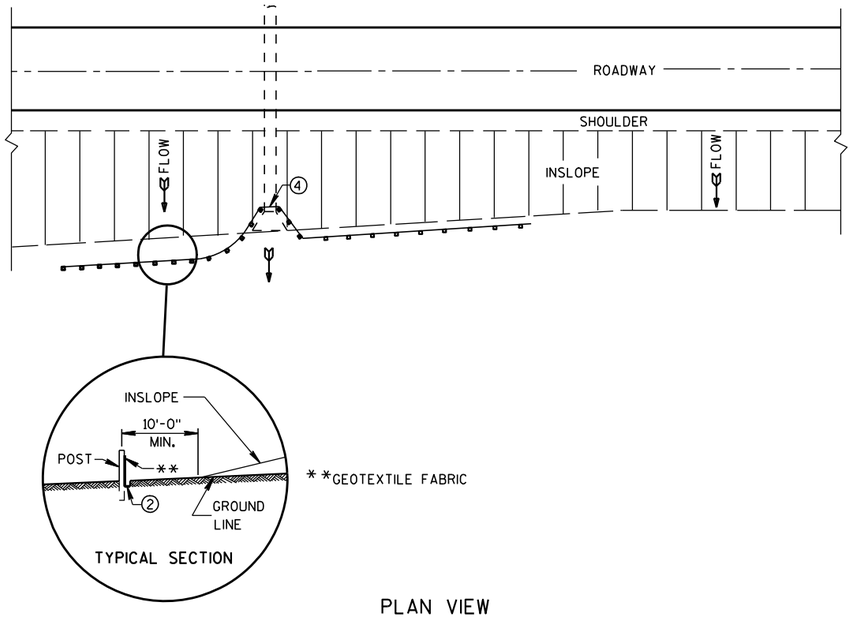
90000560-050101-pp

WISDOT/CADDS SHEET 44

# Standard Detail Drawing List

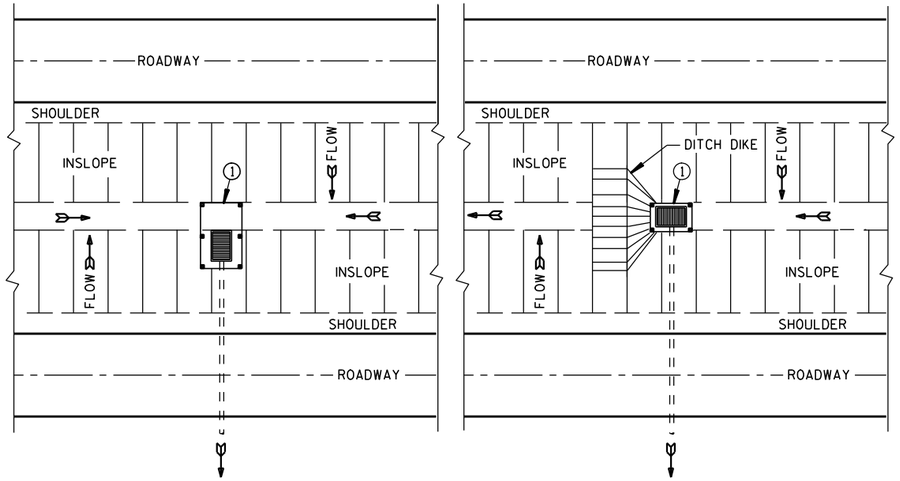
| | |
|---|---|
| 08E09-06 | SILT FENCE |
| 08F04-07 | JOINT TIES FOR CONCRETE PIPE  AND CONCRETE COLLAR DETAIL |
| 12A03-10 | NAME PLATE (STRUCTURES) |
| 15A03-02A | FLEXIBLE MARKER POST FOR CULVERT END |
| 15A03-02B | FLEXIBLE MARKER POST FOR CULVERT END |
| 15C02-06A | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-06B | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-06C | DETOUR SIGNING FOR MAINLINE CLOSURES |
| 15C03-03 | BARRICADES AND SIGNS FOR SIDEROAD CLOSURES |
| 15C04-03 | TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC |
| 15C08-17A | LONGITUDINAL MARKING (MAINLINE) |
| 15C12-05 | TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION |
| 15D28-03 | TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED  ROADWAY |
| 15D38-01A | TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS |
| 15D38-01B | ATTACHMENT OF SIGNS TO POSTS |


6



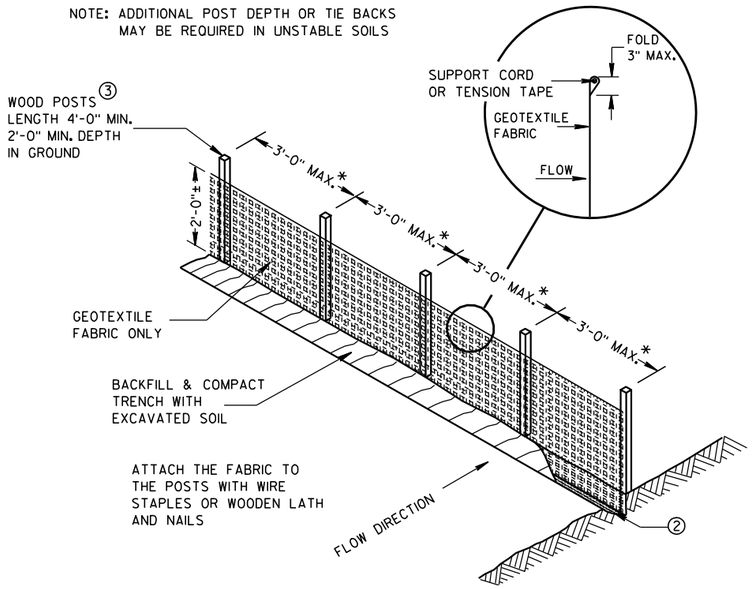

PLAN VIEW

## TYPICAL APPLICATION OF SILT FENCE



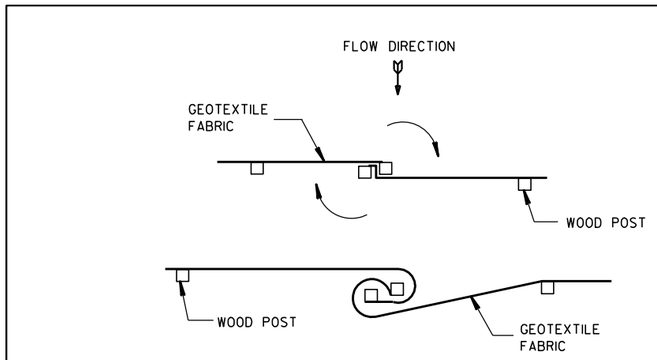

PLAN VIEW

## SILT FENCE AT MEDIAN SURFACE DRAINS




*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

## SILT FENCE

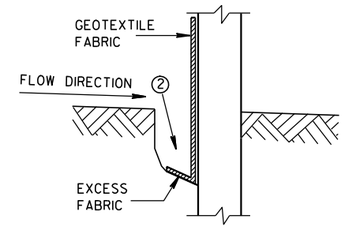



## JOINING TWO LENGTHS OF SILT FENCE 5

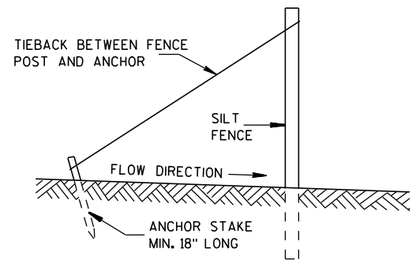
## GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.




## TRENCH DETAIL




## SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

**SILT FENCE**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

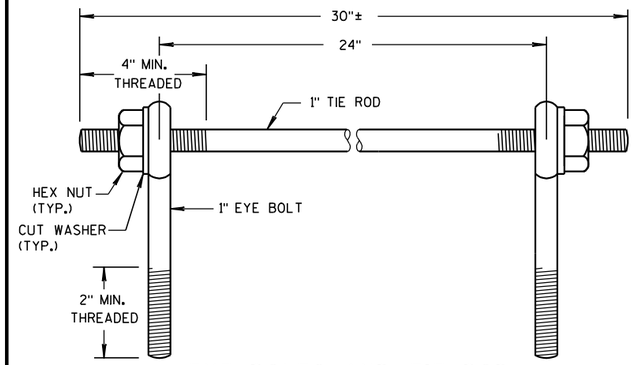


6

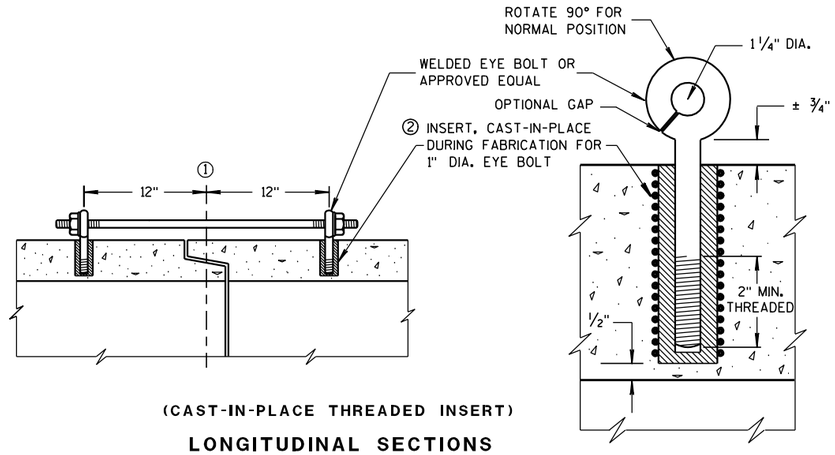
S.D.D. 8 E 9-6

6

S.D.D. 8 E 9-6




EYE BOLTS AND TIE ROD




(CAST-IN-PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

# EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

## GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

① ℄ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

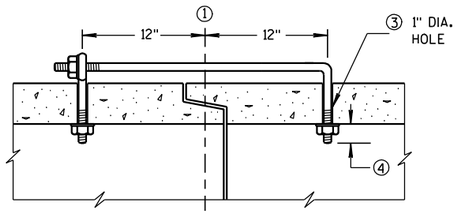
② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ℄ OF TONGUE AND GROOVE.

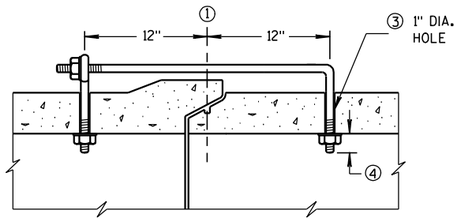
④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.

⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.




(TONGUE & GROOVE PIPE)

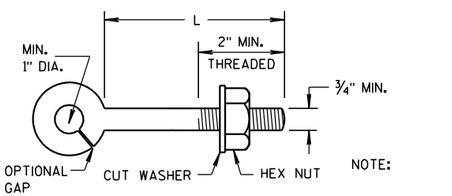



(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

**EYE BOLT DIMENSION TABLE**

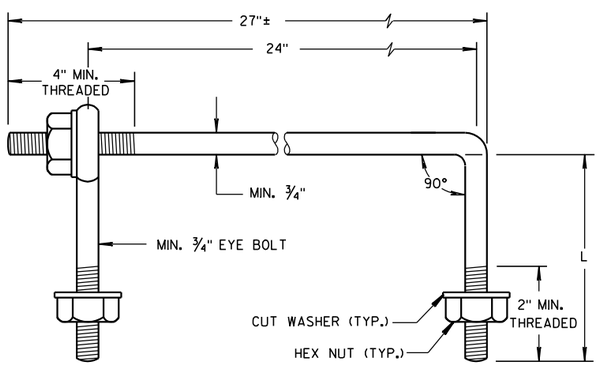
| PIPE SIZE | L = LENGTH | |
|---|---|---|
| | TONGUE & GROOVE PIPE | MODIFIED BELL PIPE |
| 18" TO 24" | 4 1/2" | 6 1/4" |
| 30" | 5" | 7" |
| 36" | 5 1/2" | 7" |
| 42" | 6" | |
| 48" | 6 1/2" | |
| 60" | 7 1/2" | |
| 66" | 8" | |




NOTE:

EYE BOLT

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.




EYE BOLT AND TIE ROD

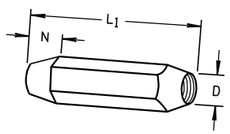
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

# EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

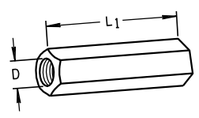
**ADJUSTABLE TIE ROD TABLE**

| PIPE DIAMETER | TIE ROD DIAMETER | D | $L_1$ | N |
|---|---|---|---|---|
| 12-60 | 5/8 | 5/8 | 5 | 1/2 |
| 66-84 | 3/4 | 3/4 | 5 | 1/2 |
| 90-108 | 1 | 1 | 7 | 1 1/16 |

DIMENSIONS SHOWN ARE IN INCHES



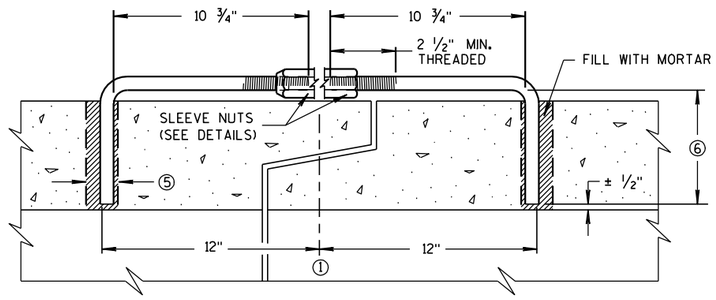

TAPERED




PLAIN

RIGHT AND LEFT THREADS

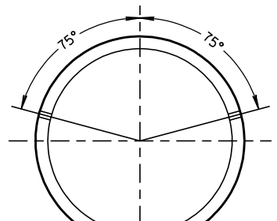
SLEEVE NUTS




LONGITUDINAL SECTION

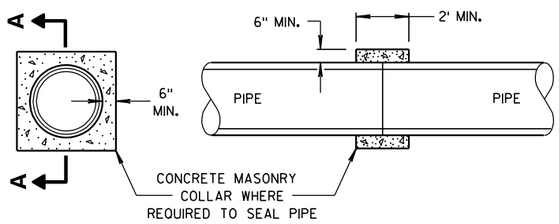
(JOINT TIES FOR 12" TO 108" DIA. CONCRETE PIPE)

# ADJUSTABLE TIE ROD (ALTERNATE NO. 3)




PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION




SECTION A-A

# CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/5/2012 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

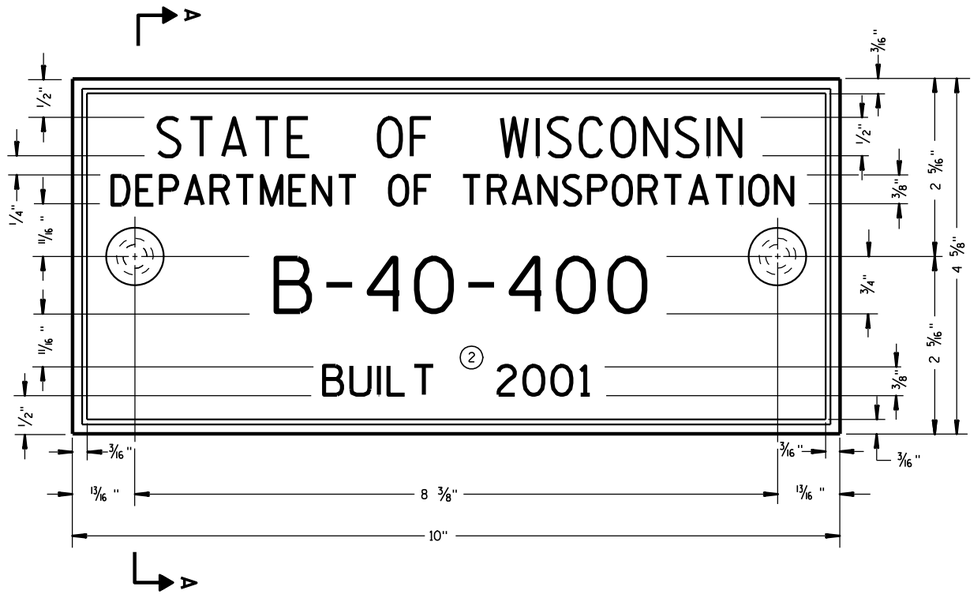


6

S.D.D. 8 F 4-7

6

S.D.D. 8 F 4-7




**TYPICAL NAME PLATE**
(BRIDGES, CULVERTS, AND RETAINING WALLS)

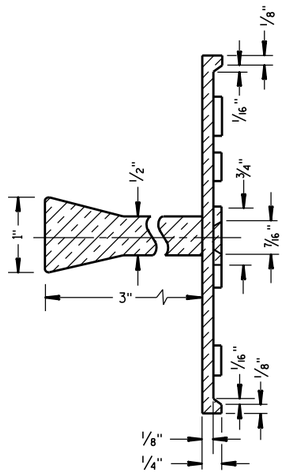
## GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

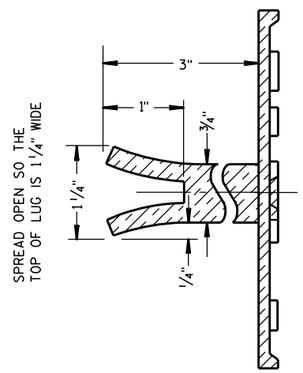
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.




**SECTION A-A**




**ALTERNATE LUG**

FOR MULTI-UNIT STRUCTURES
LINE 3 ABOVE SHALL READ

B = BRIDGE
C = CULVERT
R = RETAINING WALL

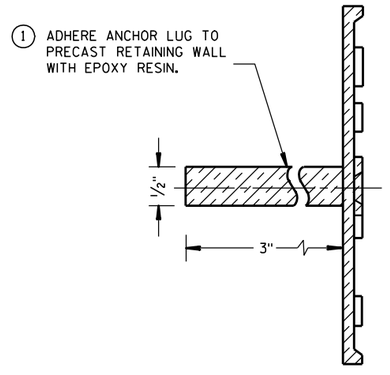
COUNTY NO.

BRIDGE NO.

UNIT NO. FOR MULTIPLE UNIT BRIDGE

B-40-400-1A

**NUMBERING DESIGNATION**
**MULTI-UNIT STRUCTURES**




**ALTERNATE LUG**
(FOR ATTACHMENT TO PRECAST STRUCTURES)

**NAME PLATE (STRUCTURES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/26/10 DATE

/S/ Scot Beck -- CHIEF STRUCTURAL DEVELOP ENGINEER

FHWA

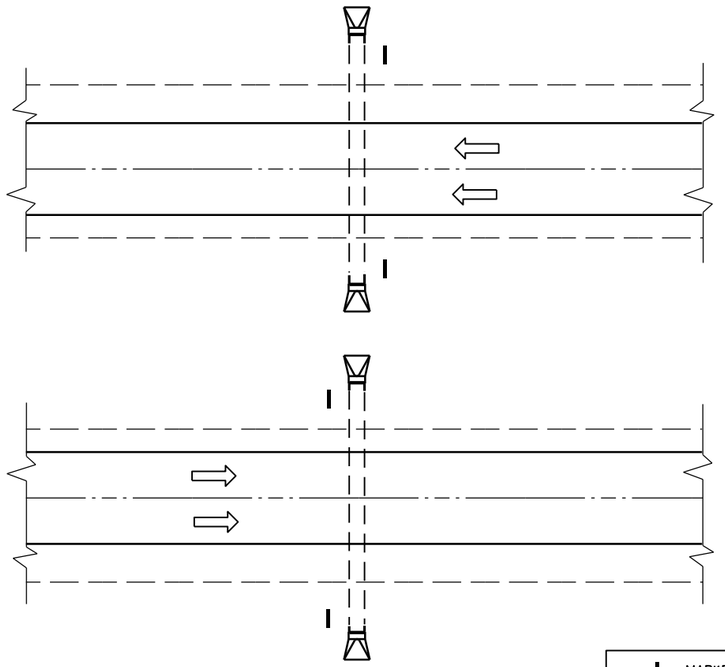


6

S.D.D. 12 A 3-10

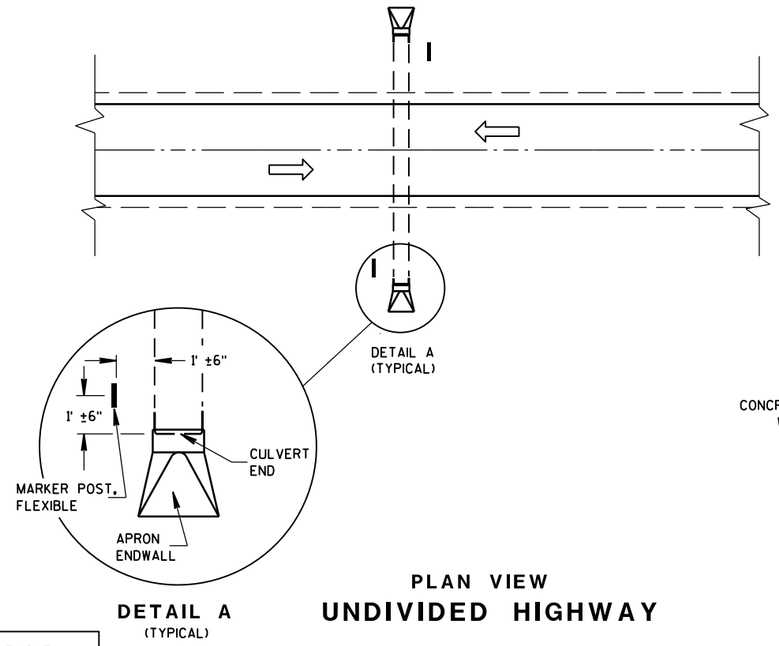
6

S.D.D. 12 A 3-10



PLAN VIEW

**DIVIDED HIGHWAY**




DETAIL A
(TYPICAL)

PLAN VIEW

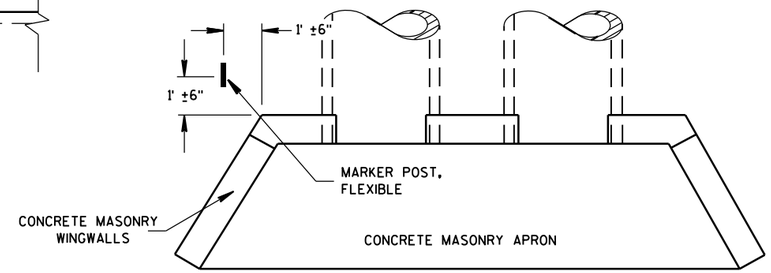
**UNDIVIDED HIGHWAY**

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

## GENERAL NOTES

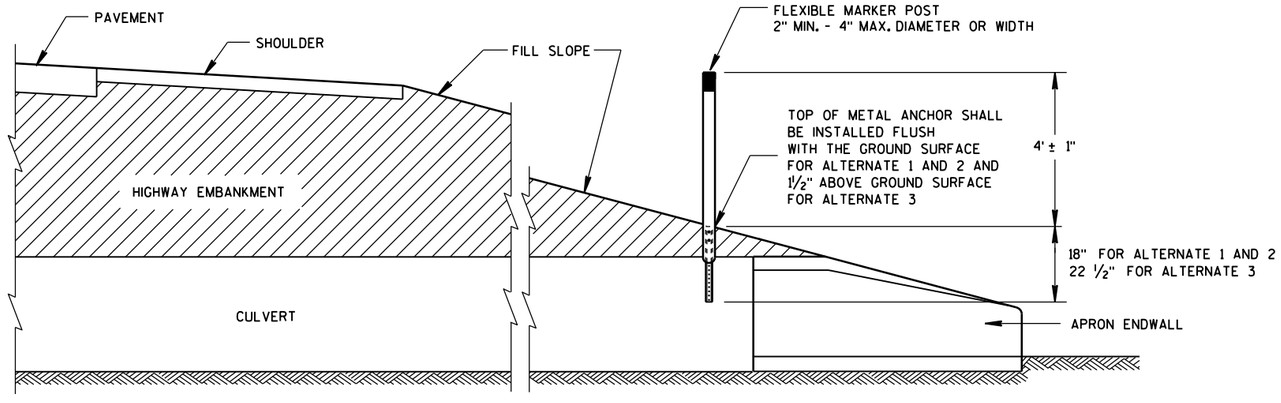
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.




PLAN VIEW

**CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH**

## FLEXIBLE MARKER POST LOCATION




CROSS SECTION

**FLEXIBLE MARKER POST**

**FLEXIBLE MARKER POST FOR CULVERT END**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

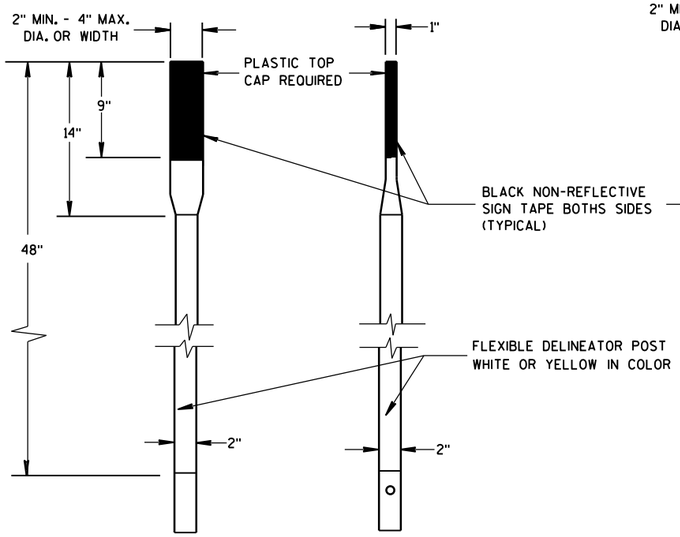


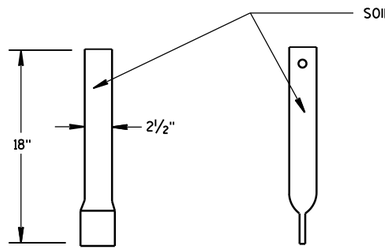
6

S.D.D. 15 A 3-2a

6

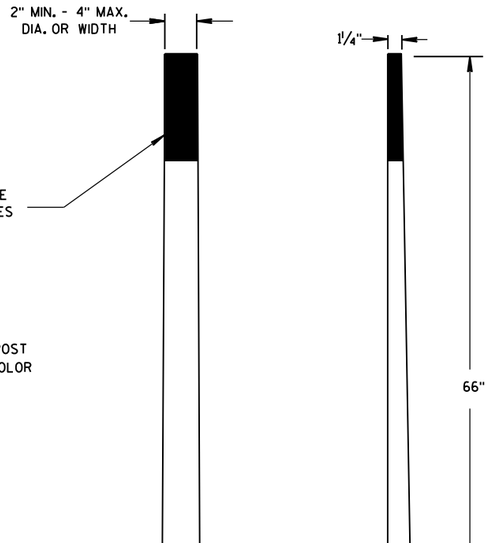
S.D.D. 15 A 3-2a



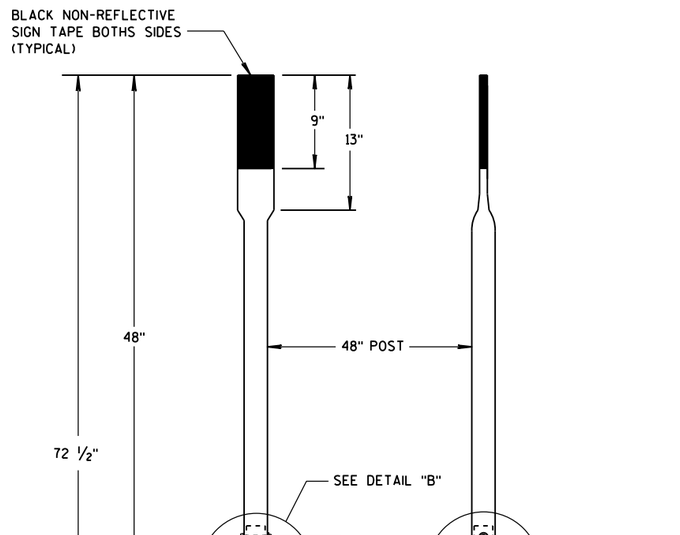

FRONT VIEW SIDE VIEW

ALTERNATE 1




FRONT VIEW SIDE VIEW

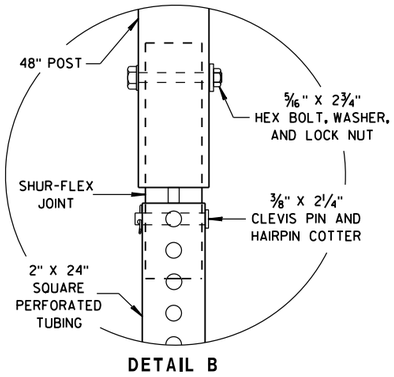
ALTERNATE 2



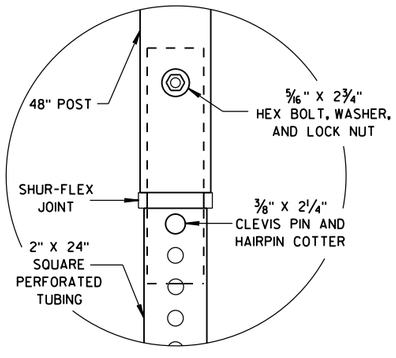

FRONT VIEW SIDE VIEW

ALTERNATE 3

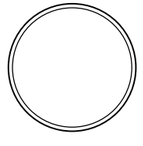
## FLEXIBLE MARKER POSTS



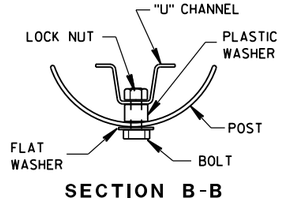

DETAIL B



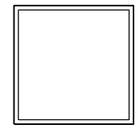

DETAIL C



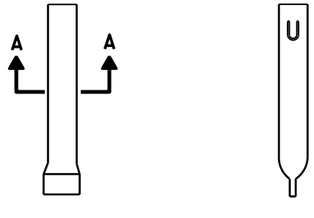
SECTION A-A




SECTION B-B

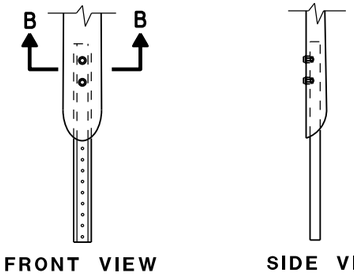


SECTION C-C



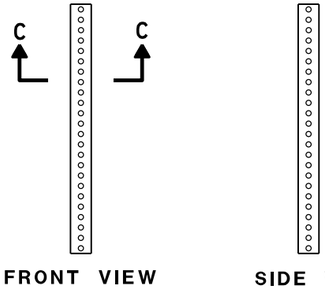

FRONT VIEW SIDE VIEW

ALTERNATE 1




FRONT VIEW SIDE VIEW

ALTERNATE 2




FRONT VIEW SIDE VIEW

ALTERNATE 3

## FLEXIBLE MARKER POST ANCHORS

**FLEXIBLE MARKER POST FOR CULVERT END**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

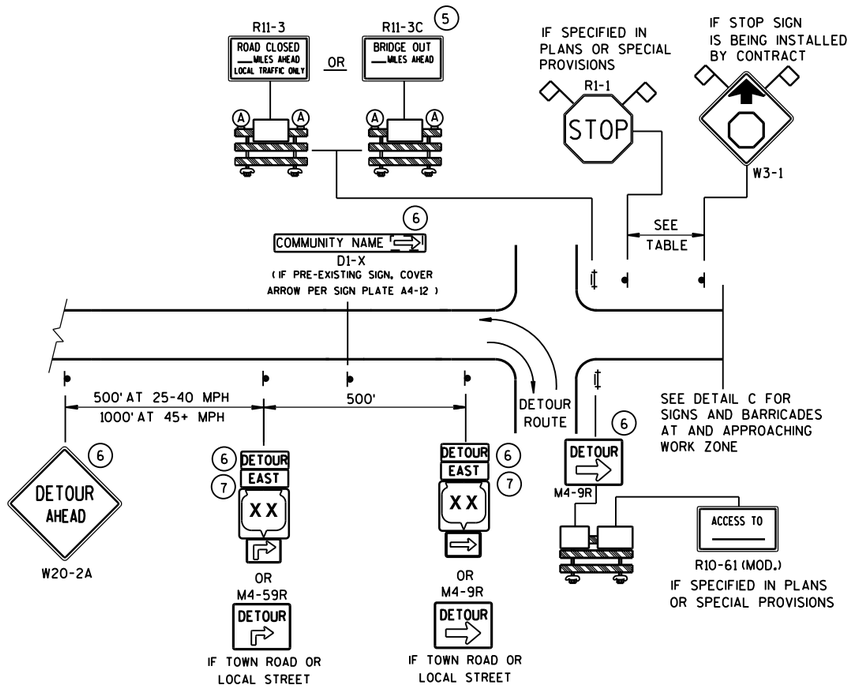


6

S.D.D. 15 A 3-2b

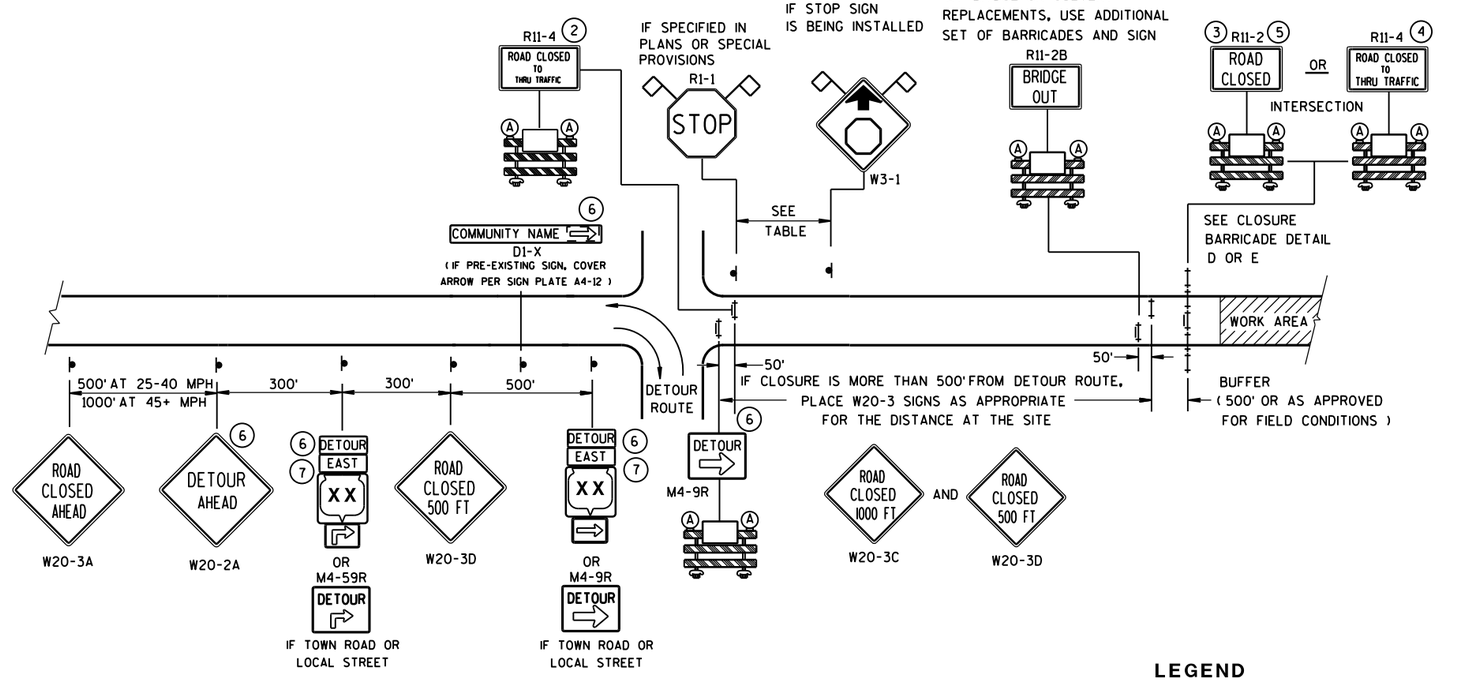
6

S.D.D. 15 A 3-2b



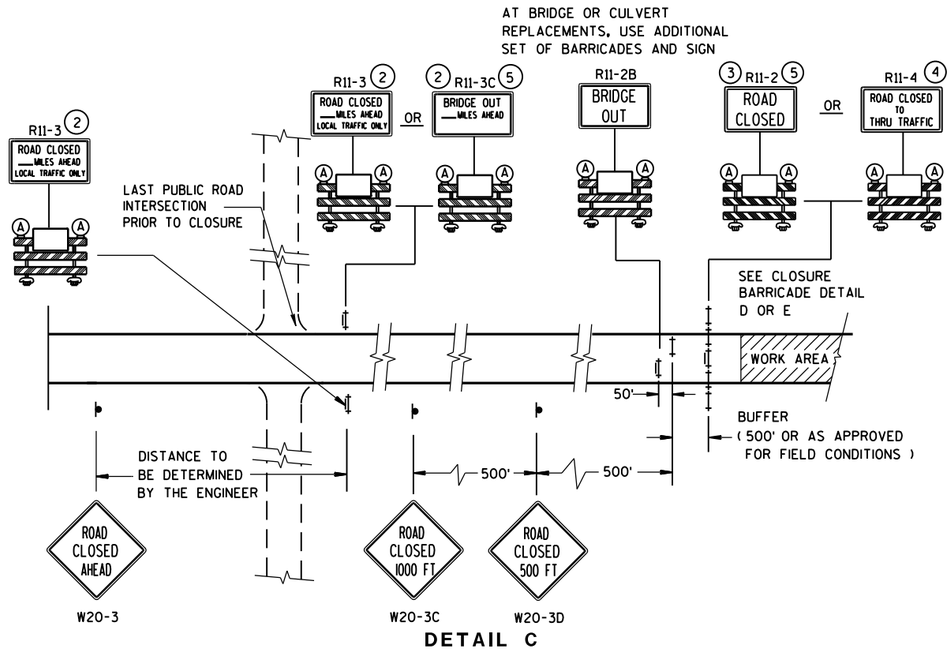

## DETAIL A
## MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE ( 1000 FEET IF URBAN )




## DETAIL B
## MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE ( 1000 FEET IF URBAN )




## DETAIL C
## MAINLINE CLOSURE, NO POSTED DETOUR

| SPEED LIMIT (MPH) | "STOP AHEAD" ADVANCE WARNING DISTANCE (FT) |
|---|---|
| 25 | 200 |
| 30 | 200 |
| 35 | 350 |
| 40 | 350 |
| 45 | 500 |
| 50 | 550 |
| 55 | 750 |

SEE SDD 15C2-SHEET "**b**"
FOR GENERAL NOTES
AND FOOTNOTES (1) THROUGH (7)

### LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

**BARRICADES AND SIGNS FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA

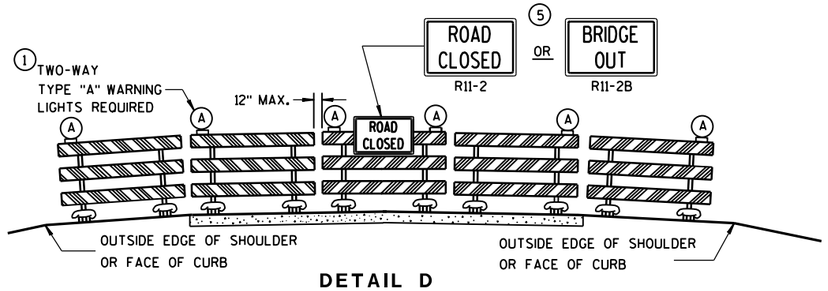


6

S.D.D. 15 C 2-6a

6

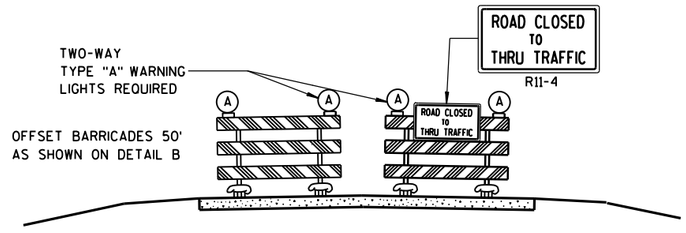
S.D.D. 15 C 2-6a




**DETAIL D**

## ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW




**DETAIL E**

## LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

## GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 — DATE

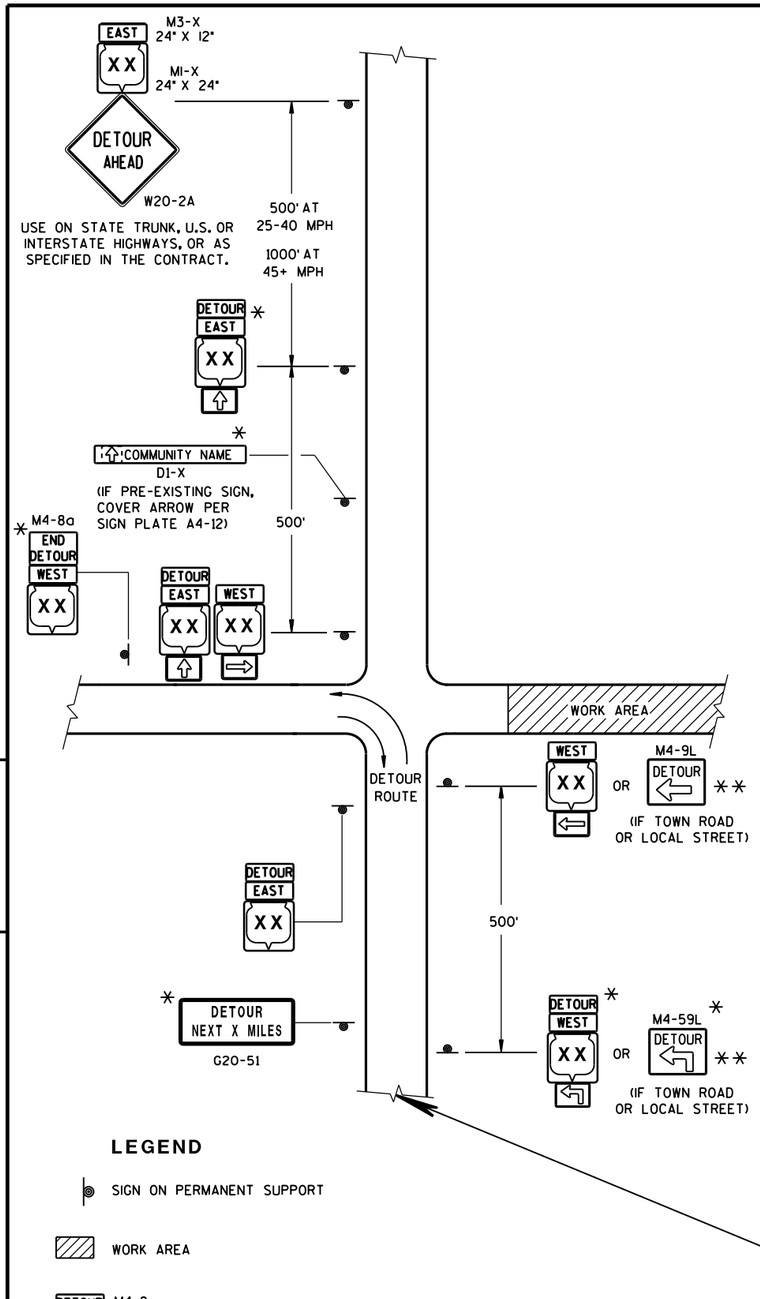
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO SAFETY ENGII — FHWA — FIC



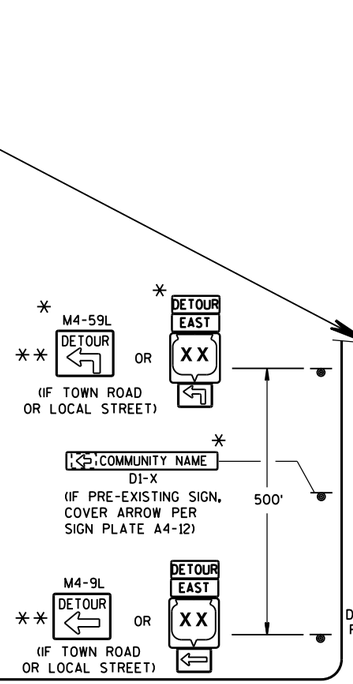

6

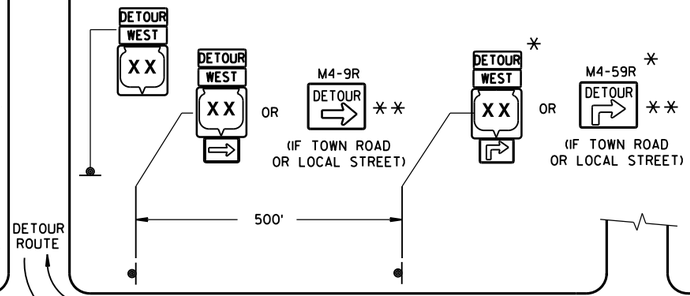

S.D.D. 15 C 2-6b

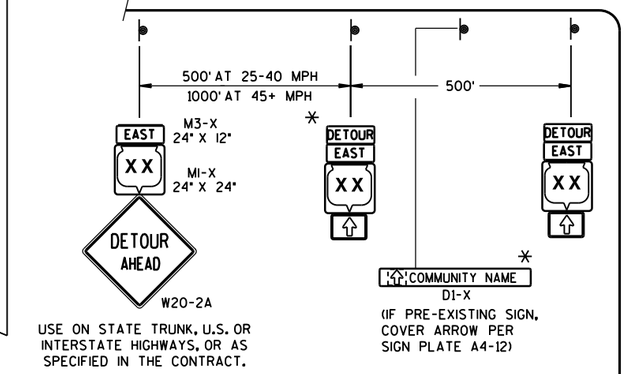
**THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.**

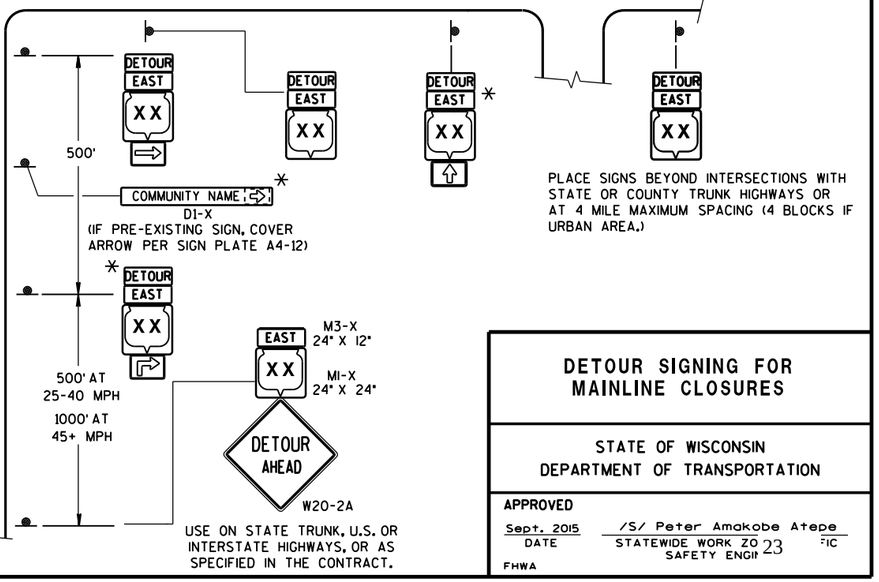



**MATCH POINT**




## LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR EAST — M4-8, M3-X
- XX OR COUNTY XX OR XX — M1-4, M1-5A, M1-6
- M05-1 OR M06-1 OR M06-1

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD 15C2-SHEET "a"

## GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-9 SHALL BE 30" X 24".
- M4-8a SHALL BE 24" X 18".
- G20-51 SHALL BE 60" X 24".
- W20-2 SHALL BE 48" X 48".
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

**DETAIL F**

**DETOUR SIGNING**

**DETOUR SIGNING FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

Sept. 2015 — DATE

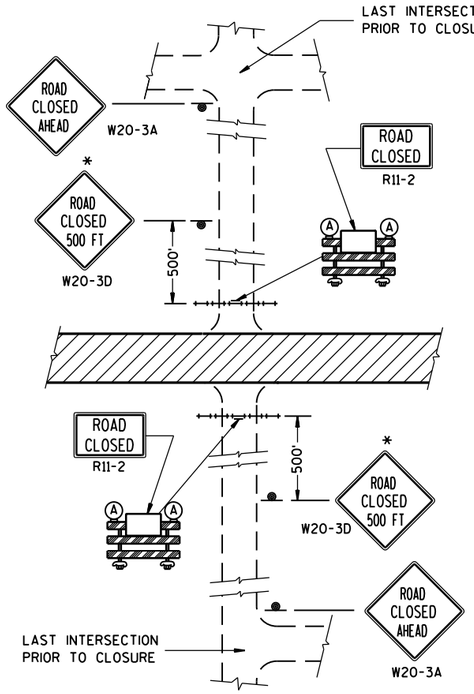
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO... SAFETY ENGI... FIC

FHWA



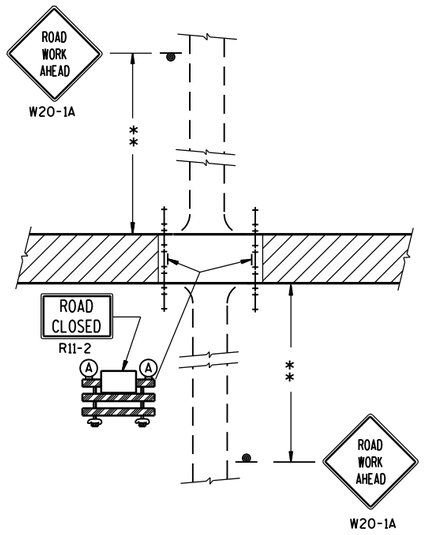
6

S.D.D. 15 C 2-6c



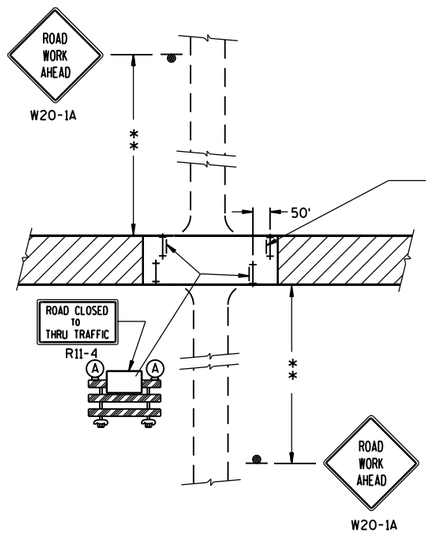

**DETAIL 1**

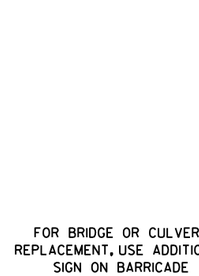
(NO ACCESS TO PROJECT)

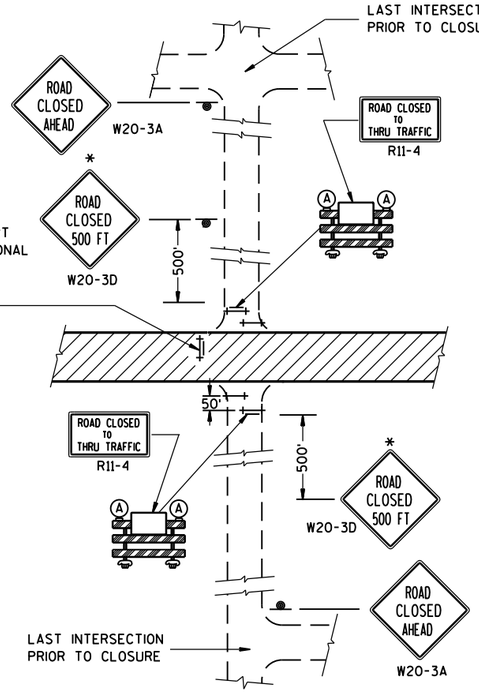



**DETAIL 2**

(PUBLIC CROSS-TRAFFIC MAINTAINED. NO ACCESS TO PROJECT).




**DETAIL 3**

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).

**DETAIL 4**

(CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS TO PROJECT)

## GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

## LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

**BARRICADES AND SIGNS FOR SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO[illegible]FIC SAFETY ENGI[illegible]
FHWA



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S.D.D. 15 C 3-3

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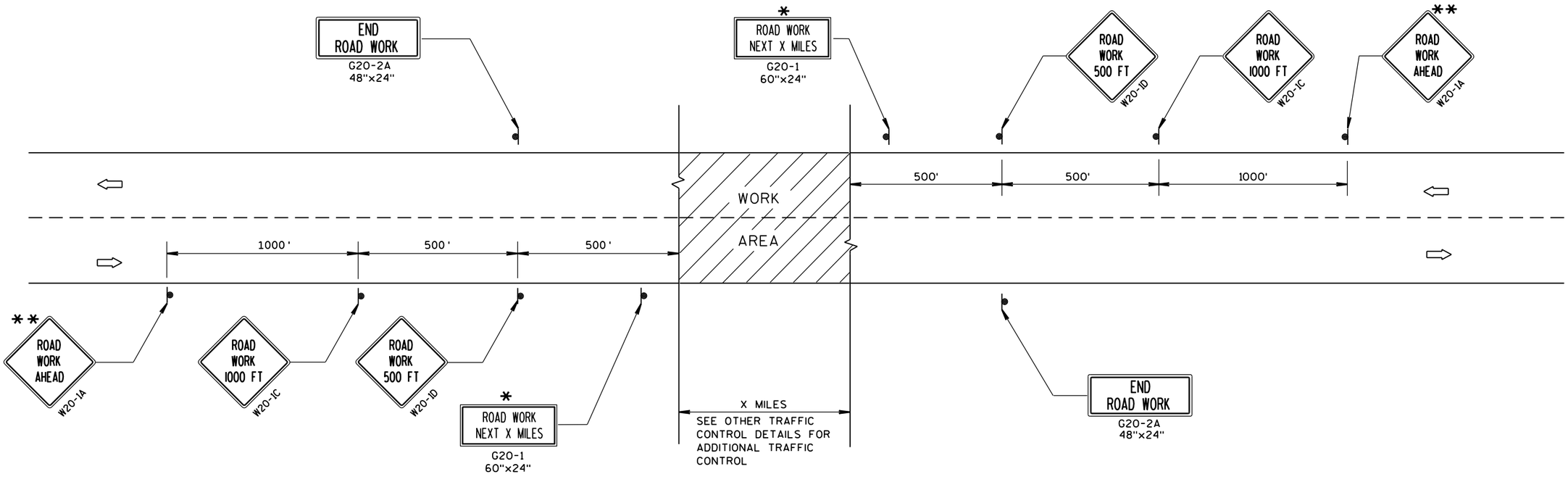
S.D.D. 15 C 3-3

# TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL




## GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

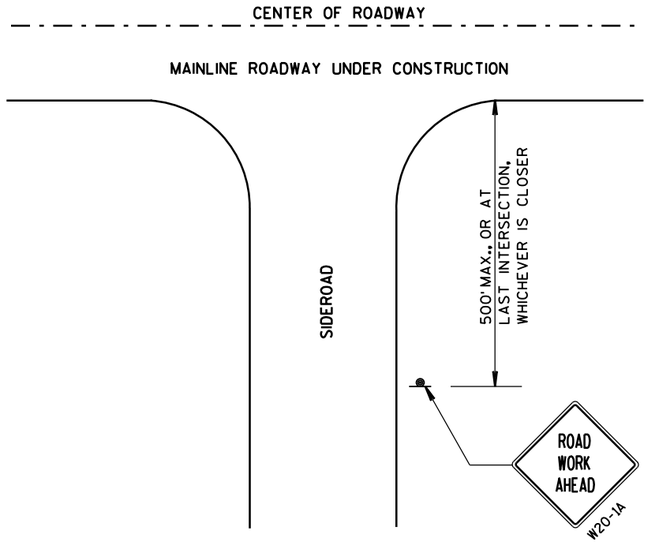
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

** PLACE ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.




### LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

| Sept. 2015 | /S/ Peter Amakobe Atepe |
|---|---|
| DATE | STATEWIDE WORK ZONE SAFETY ENGINEER |

FHWA





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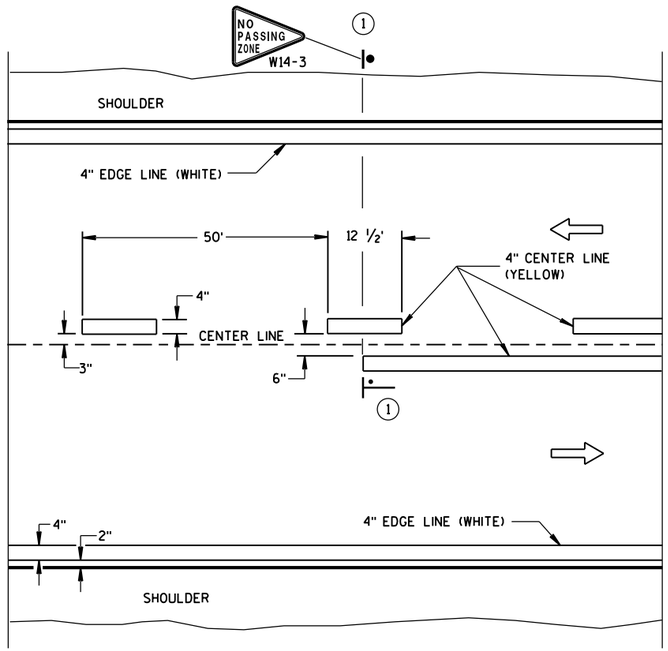
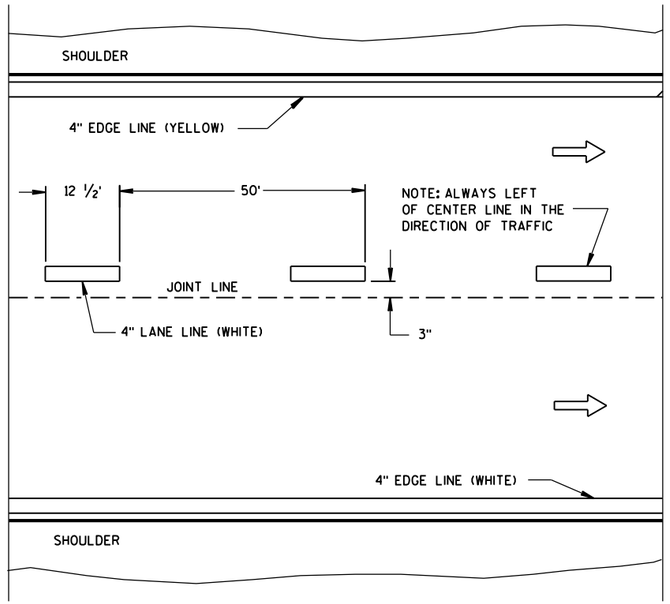
S.D.D. 15 C 4-3




TWO WAY TRAFFIC




ONE WAY TRAFFIC

## PERMANENT PAVEMENT MARKING

## GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) NO PASSING ZONE W14-3 SIGN SHALL BE LOCATED WITHIN 50 FEET OF THE "T" MARKING.

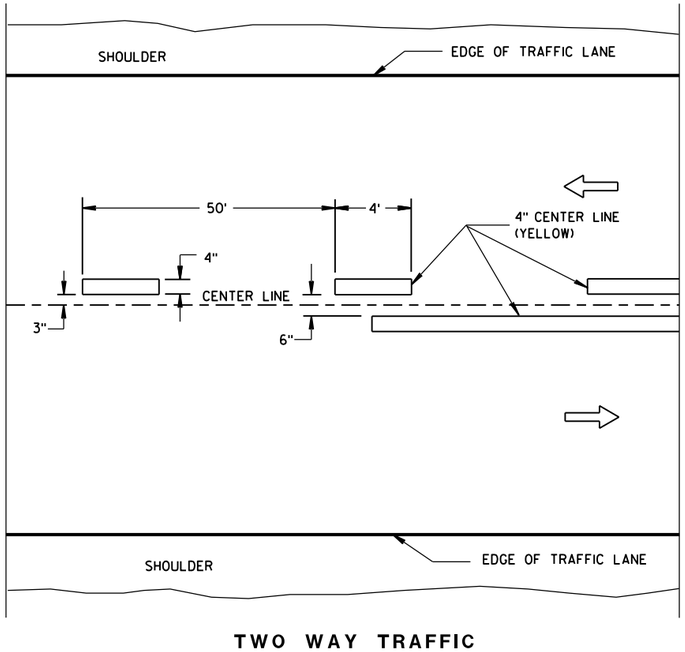
## NOTE

ARROW SYMBOL ( ⇨ ) SHOWS DIRECTION OF TRAVEL

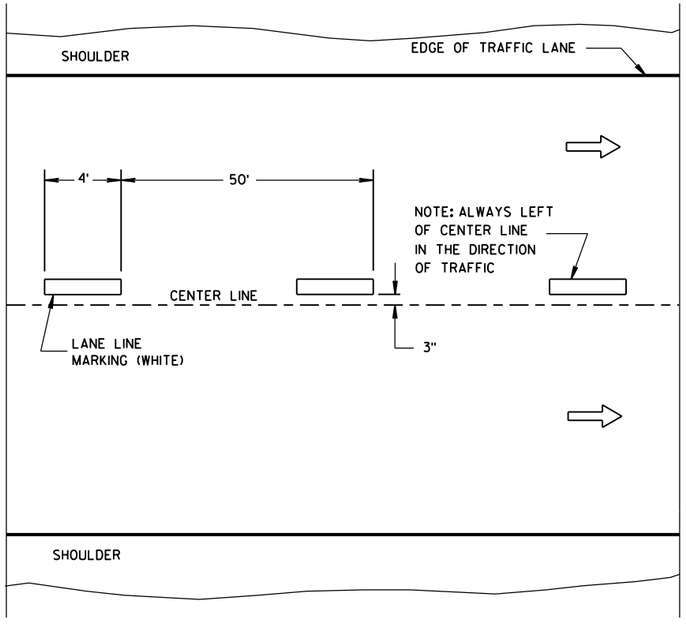
## LEGEND

"T" MARKING

POST MOUNTED SIGN




TWO WAY TRAFFIC




ONE WAY TRAFFIC

## TEMPORARY PAVEMENT MARKING

| LONGITUDINAL MARKING (MAINLINE) | |
|---|---|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED | |
| Sept., 2016 | /S/ Matthew R. Rauch |
| DATE | STATE SIGNING AND MAR ENGINEER |
| FHWA | |

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S.D.D. 15 C 8-17a

## LEGEND

SIGN ON PORTABLE OR PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA

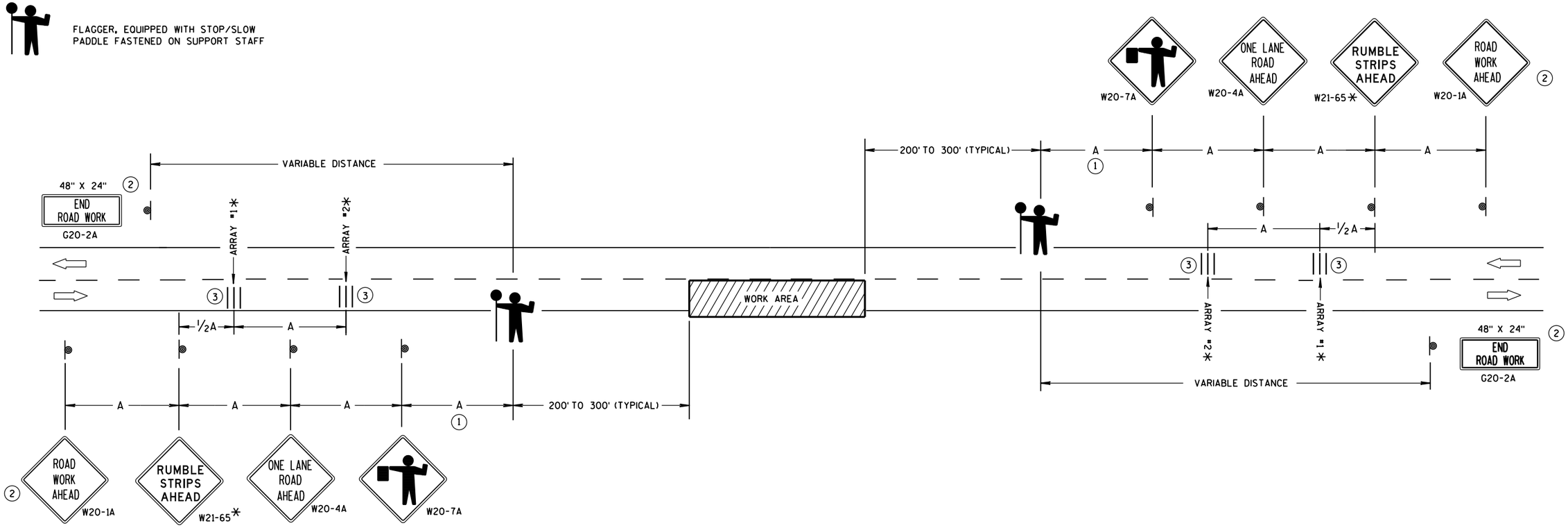
FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

## SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

| SPEED LIMIT | SPACING A |
|---|---|
| 25-35 MPH | 200' |
| 35-40 MPH | 350' |
| 45-55 MPH | 500' |

BE PREPARED TO STOP
W03-4

USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING A.




# TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

## GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* UTILIZE TEMPORARY RUMBLE STRIPS WHEN FLAGGING OPERATION IS ANTICIPATED TO BE STATIONARY IN EXCESS OF TWO HOURS.

(1) FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS (IF USED) SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

(3) EACH TEMPORARY RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.

**TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December, 2016 — /S/ Andrew Heldtke
DATE — WORK ZONE ENG
FHWA



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S.D.D. 15 C 12-5

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S.D.D. 15 C 12-5

## GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

## TABLE A

| S \ W | SHOULDER TAPER LENGTH (FEET) 4 | 6 | 8 | 10 | BUFFER SPACE (FEET) |
|---|---|---|---|---|---|
| 30 | 20 | 30 | 40 | 50 | 200 |
| 35 | 30 | 45 | 55 | 70 | 250 |
| 40 | 40 | 55 | 75 | 90 | 305 |
| 45 | 60 | 90 | 120 | 150 | 360 |
| 50 | 70 | 100 | 135 | 170 | 425 |
| 55 | 75 | 110 | 150 | 185 | 495 |

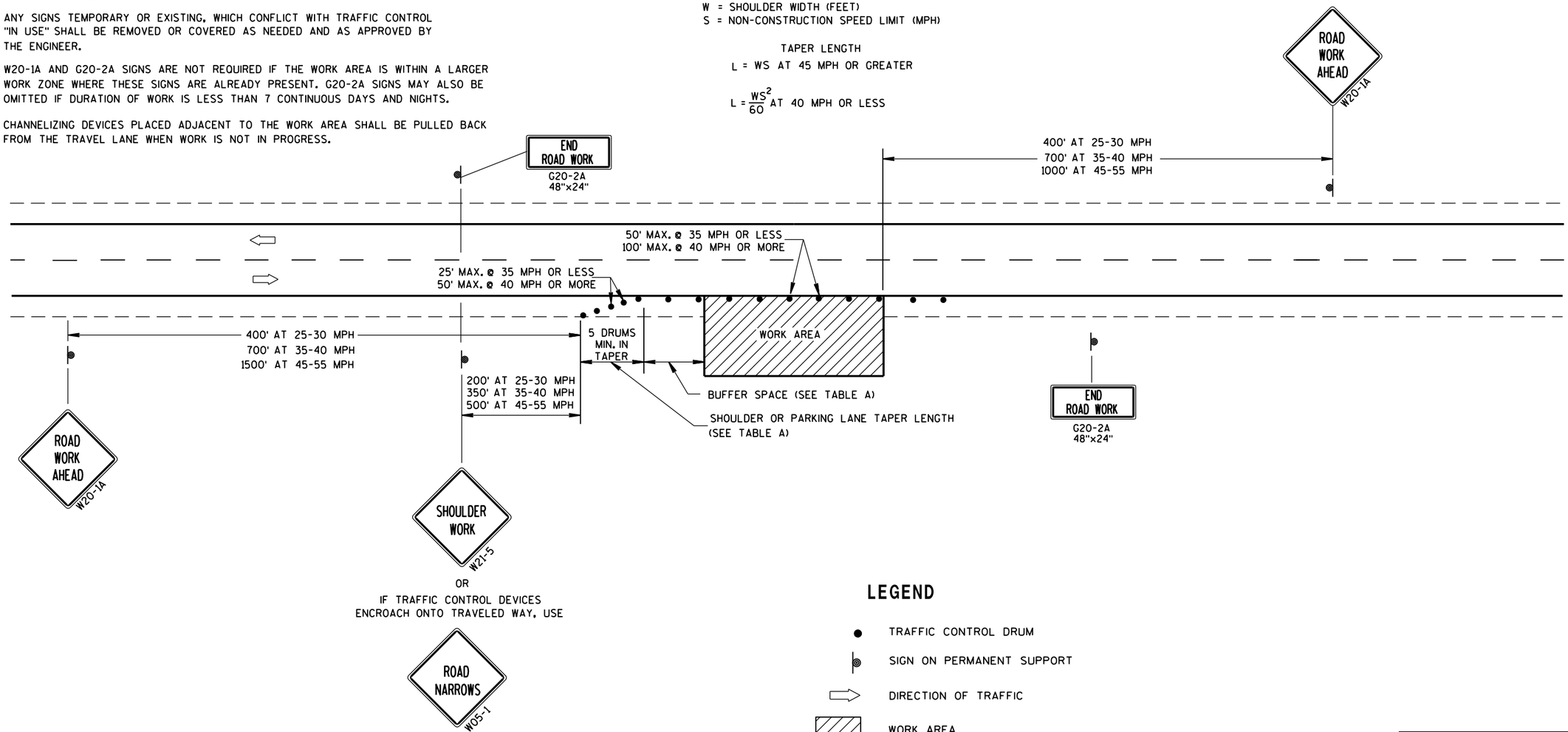
W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH

L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = $\frac{1}{3}L$




## LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

**TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

July 14, 2015 — DATE

/S/ Peter Amakobe Atepe — STATEWIDE WORK ZO SAFETY ENGI FIC

FHWA





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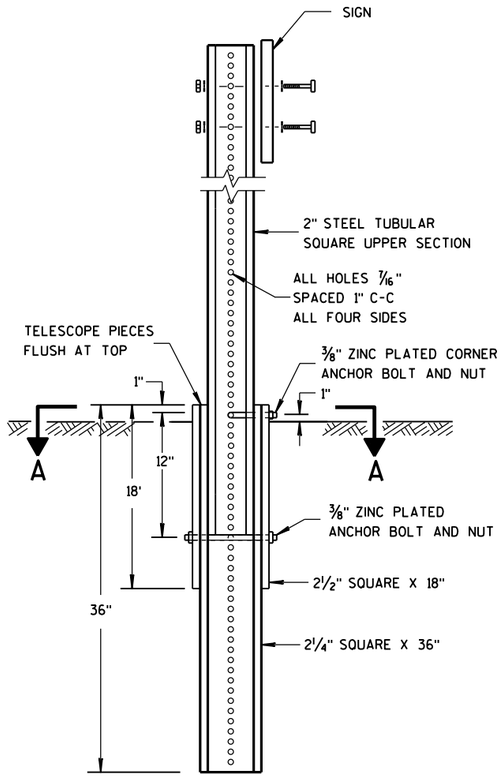
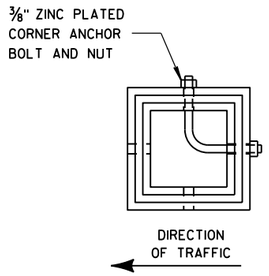
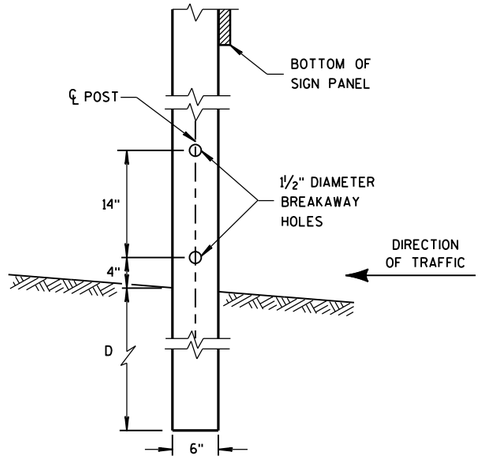
S.D.D. 15 D 28-3




## DETAIL OF TUBULAR STEEL SIGN POST




## SECTION A-A



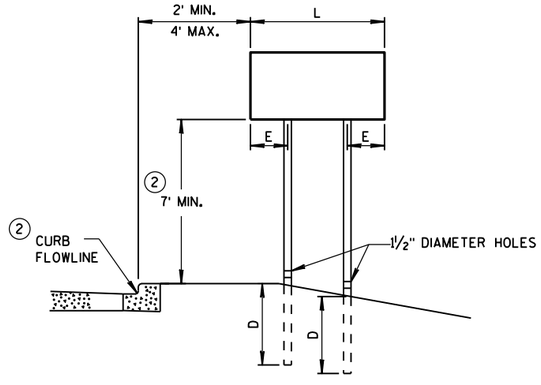

## 4"x6" WOOD POST MODIFICATION

## GENERAL NOTES

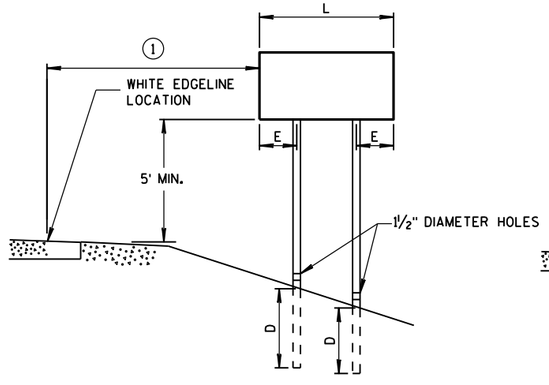
(1) 6 FEET FROM THE EDGE OF PAVEMENT (EDGE LINE LOCATION) UNLESS OTHERWISE DIRECTED BY THE PROJECT ENGINEER. LATERAL OFFSET SHOULD BE ADJUSTED TO AVOID THE DITCH FLOWLINE.

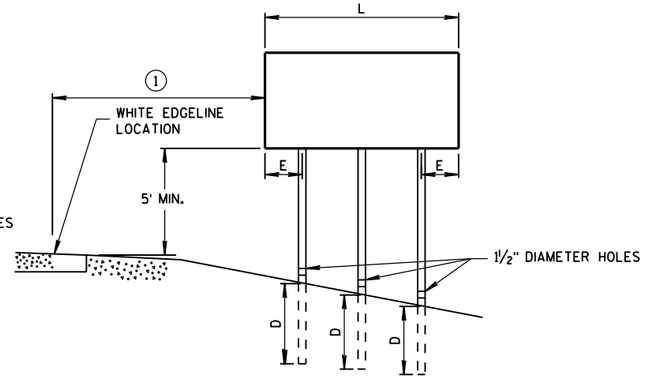
(2) THE EXISTENCE OF CURB AND GUTTER DOES NOT IN ITSELF MANDATE THE VERTICAL CLEARANCE ILLUSTRATED. THAT HEIGHT IS TYPICALLY MEASURED WHERE THERE IS SIDEWALK ADJACENT TO THE ROADWAY OR PARKING IS PERMITTED. IN THE ABSENCE OF SIDEWALK, VERTICAL CLEARANCE IS MEASURED FROM THE TOP OF THE CURB. IF NO SIDEWALK AND NO PARKING, VERTICAL CLEARANCE MAY BE REDUCED TO 5 FOOT MINIMUM. OFFSET OF SIGNS IS MEASURED FROM THE CURB FLOWLINE.

(3) FOR SIGNS REQUIRING 4 POSTS, SPACE INTERMEDIATE POSTS EVENLY.




## URBAN AREA




## RURAL AREA

## POST MOUNTING DETAIL FOR TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

### TUBULAR STEEL POSTS

| AREA OF SIGN INSTALLATION (SQ. FT.) | NUMBER OF REQUIRED TUBULAR STEEL POSTS |
|---|---|
| 9 OR LESS | 1 |
| GREATER THAN 9 LESS THAN OR EQUAL TO 18 | 2 |
| GREATER THAN 18 LESS THAN OR EQUAL TO 27 | 3 |

SIGNS WIDER THAN 3 FEET OR LARGER THAN 9 SQ. FT. SHALL BE MOUNTED ON MULTIPLE POSTS (SEE ABOVE TABLE).

SIGNS LARGER THAN 27 SQ. FT. SHALL NOT BE MOUNTED ON TUBULAR STEEL POSTS.

### WOOD POST EMBEDMENT DEPTH

| AREA OF SIGN INSTALLATION (SQ. FT.) | D (MIN) |
|---|---|
| 20 OR LESS | 4' |
| GREATER THAN 20 | 5' |

### 4" X 6" WOOD POST

| POST SPACING REQUIREMENTS | | NUMBER OF WOOD POSTS REQUIRED |
|---|---|---|
| L | E | |
| 48" OR LESS AND LESS THAN 20 SQ. FT. | - | 1 |
| LESS THAN 60" | 12" | 2 |
| 60" TO 120" | L/5 | 2 |
| GREATER THAN 120" LESS THAN 168" | 12" | 3 |
| 168" AND GREATER | 12" | 4 |

SEE NOTE (3)

TEMPORARY TRAFFIC CONTROL FIXED MESSAGE SIGNS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI





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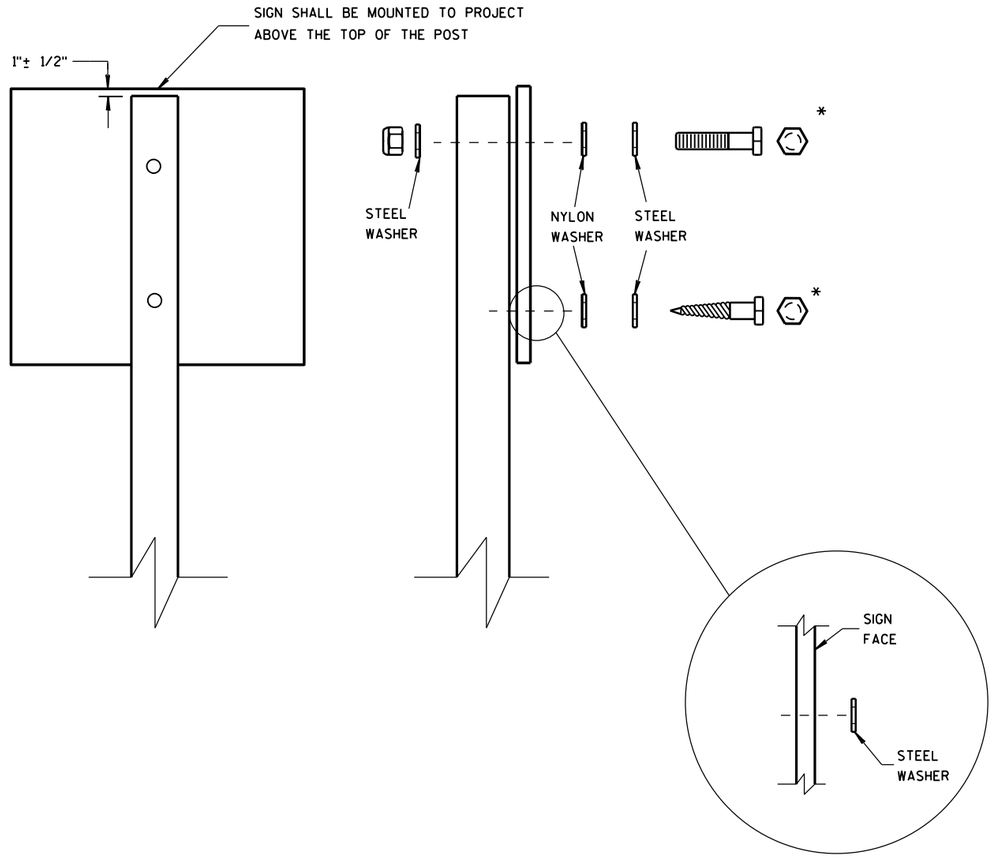
S.D.D. 15 D 38-1a




WASHER PLACEMENT WHEN SIGN HAS OTHER THAN TYPE H OR TYPE F FACE

NUTS, BOLTS AND LAGS USED FOR MOUNTING SIGNS SHALL HAVE HEXAGONAL HEADS AND SHALL BE EITHER:

A. HOT DIP GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: A 153, CLASS D, OR SC 3

B. ELECTRO-GALVANIZED IN ACCORDANCE WITH ASTM DESIGNATION: B 633, TYPE III, SC 3

THREADS ON BOLTS AND NUTS SHALL BE MANUFACTURED WITH SUFFICIENT ALLOWANCE FOR THE CADMIUM PLATE OR GALVANIZED COATING TO PERMIT THE NUTS TO RUN FREELY ON THE BOLTS.

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3"

MACHINE BOLTS - 5/16" X 6-1/2" OR 7" LENGTH W/ NUTS

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" LENGTH W/ NUTS

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON FOR ALL TYPE H SIGNS

* TWO DIFFERENT FASTENING SYSTEMS ARE SHOWN FOR ILLUSTRATION PURPOSES. ON ANY INDIVIDUAL SIGN, EITHER ONE OR THE OTHER SYSTEM SHALL BE USED. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA. FOR A SINGLE POST INSTALLATION, ALL SIGNS GREATER THAN 9 SQ. FT. REQUIRE THE USE OF 3 FASTENERS.

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S.D.D. 15 D 38-1b

S.D.D. 15 D 38-1b

# ATTACHMENT OF SIGNS TO POSTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

Feb. 2015 — DATE

/S/ Travis Feltes — STATE TRAFFIC ENGINEER

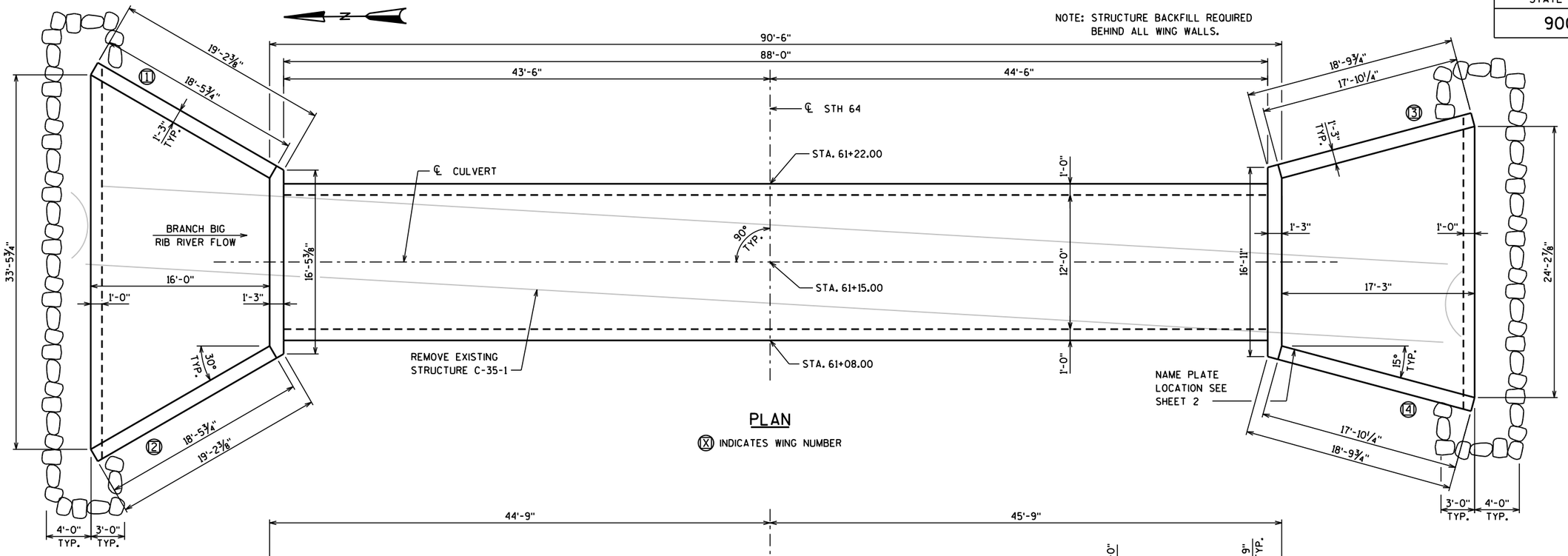
FHWA



SIGN

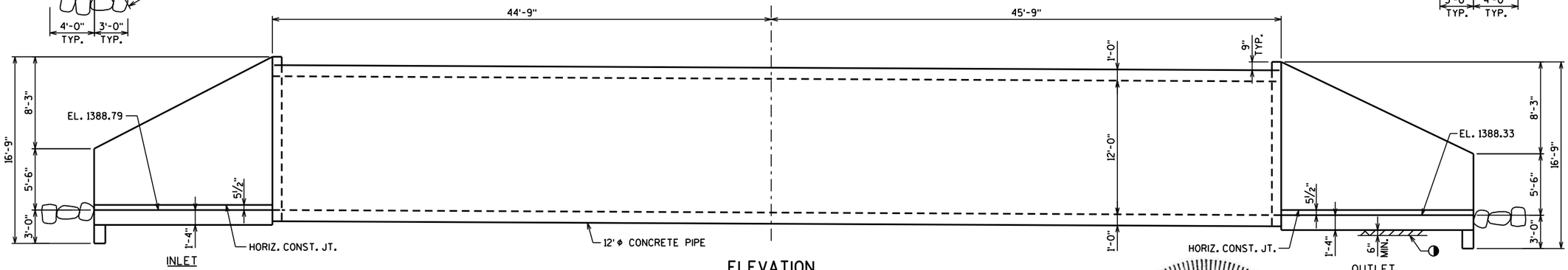
STATE PROJECT NUMBER
9000-05-60

NOTE: STRUCTURE BACKFILL REQUIRED BEHIND ALL WING WALLS.




## PLAN

Ⓧ INDICATES WING NUMBER




## ELEVATION

(LOOKING EAST)

## HYDRAULIC DATA

### 100 YEAR FREQUENCY

$Q_{100}$ ——— 134 C.F.S.
VELOCITY ——— 7.25 F.P.S.
HIGH WATER ——— EL. 1395.86 (100 YEAR)
WATERWAY AREA ——— N/A S.F.
DRAINAGE AREA ——— 0.92 SQ. MILES
OVERTOPPING FREQUENCY = N/A
SCOUR CRITICAL CODE = 5

### 2 YEAR FREQUENCY

$Q_2$ ——— 37 C.F.S.
HIGH WATER ——— EL. 1394.31 (2 YEAR)

## DESIGN DATA

### MATERIAL PROPERTIES:

CONCRETE MASONRY
GRADE A-FA ——— f'c = 3,500 PSI
HIGH STRENGTH BAR STEEL
REINFORCEMENT, GRADE 60 ——— fy = 60,000 PSI

### EARTH LOAD:

ENDWALL DESIGNED FOR 1' SURCHARGE.

## GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE UPPER LIMITS OF THE "EXCAVATION FOR STRUCTURES CULVERTS C-35-19" SHALL BE THE EXISTING GROUND LINE.

ALL SPACES EXCAVATED AND NOT OCCUPIED BY THE NEW CULVERT AND APRONS SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TO THE TOP OF THE CULVERT AS SHOWN IN THE ROADWAY PLAN CONSTRUCTION DETAILS.

THE APRONS AND WINGWALLS SHALL BE CAST IN PLACE.

THE CONCRETE IN THE CUTOFF WALLS MAY BE PLACED UNDERWATER IF THE EXCAVATION CANNOT BE DEWATERED.

◑ UNDERCUT CULVERT AND APRONS 6" (INCLUDED IN EXCAVATION FOR STRUCTURES), PLACE GEOTEXTILE FABRIC TYPE 'C', AND BACKFILL WITH STRUCTURE BACKFILL. EXTEND 3'-0" BEYOND THE FOOTPRINT OF THE APRON AND AS SHOWN IN THE ROADWAY PLAN CONSTRUCTION DETAILS.

## LIST OF DRAWINGS

1. LAYOUT
2. SOUTH APRON DETAILS
3. NORTH APRON DETAILS
4. SUBSURFACE EXPLORATION

## TRAFFIC DATA

ADT = 680 (2017)
800 (2037)
RDS = 60 M.P.H.

WISCONSIN
KRISTOFER R. OLSON
E-35236
APPLETON, WI
PROFESSIONAL ENGINEER

3/28/17

## TOTAL ESTIMATED QUANTITIES

| | | | |
|---|---|---|---|
| 203.0200 | REMOVING OLD STRUCTURE STA 61+15 | 1 | LS |
| 206.2000 | EXCAVATION FOR STRUCTURES CULVERTS C-35-19 | 1 | LS |
| 210.2500 | BACKFILL STRUCTURE TYPE B | 1,040 | TON |
| 504.0100 | CONCRETE MASONRY CULVERTS | 77 | CY |
| 505.0400 | BAR STEEL REINFORCEMENT HS STRUCTURES | 1,390 | LB |
| 505.0600 | BAR STEEL REINFORCEMENT HS COATED STRUCTURES | 6,080 | LB |
| 606.0300 | RIPRAP HEAVY | 33 | SF |
| 645.0105 | GEOTEXTILE TYPE C | 90 | SY |
| 645.0120 | GEOTEXTILE TYPE HR | 70 | SY |
| SPV.0090.01 | CULVERT PIPE REINFORCED CONCRETE 144-INCH INSTALLED | 90.5 | LF |

## CONSULTANT CONTACT


KRISTOFER OLSON
OMNNI ASSOCIATES, INC.
(920) 735-6900


## BRIDGE OFFICE CONTACT


WILLIAM DREHER
(608) 266-8489


| NO. | DATE | REVISION | BY |
|---|---|---|---|
| | | | |

ORIGINAL PLANS PREPARED BY
OMNNI ASSOCIATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
ACCEPTED William C. Dreher SDR 03/31/17
CHIEF STRUCTURES DESIGN ENGINEER DATE

# STRUCTURE C-35-19

STH 64 OVER BRANCH BIG RIB RIVER

| | | | |
|---|---|---|---|
| COUNTY | LINCOLN | TOWN | CORNING |
| DESIGN SPEC. | AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS | LOAD | HL-93 |
| DESIGNED BY | BRE | DESIGN CK'D. | KRO |
| DRAWN BY | BRE | PLANS CK'D. | KRO |

LAYOUT

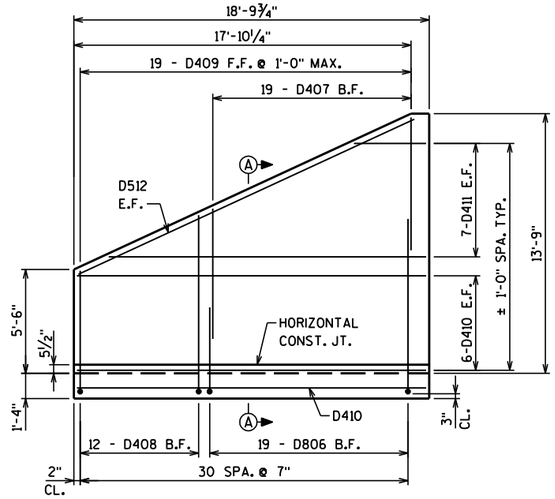
SHEET 1 OF 4

31

8

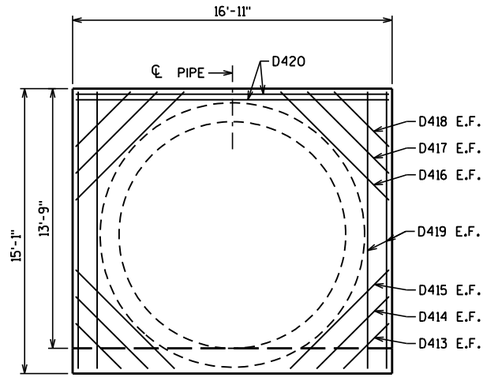
8

| STATE PROJECT NUMBER |
|---|
| 9000-05-60 |




## WING 4 ELEVATION

(WING 3 SIMILAR)




## BACKWALL FRONT FACE ELEVATION

## NOTES:

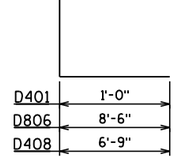
PLACE BARS AS SHOWN. SPACE AT 1'-0" ± UNLESS NOTED OTHERWISE.

(X) INDICATES WING NUMBER

## BILL OF BARS

| BAR MARK | COAT | NO. REQ'D. | LENGTH | SERIES | BENT | LOCATION |
|---|---|---|---|---|---|---|
| D401 | | 28 | 3'-6" | | X | OUTLET CUTOFF WALL |
| D402 | | 4 | 24'-2" | | | OUTLET CUTOFF WALL |
| D403 | | 20 | 16'-11" | | | OUTLET APRON |
| D404 | | 8 | 8'-2" | △ | | OUTLET APRON |
| D405 | | 17 | 21'-6" | △ | | OUTLET APRON |
| D806 | X | 38 | 15'-2" | △ | X | OUTLET WINGS CORNER B.F. |
| D407 | X | 38 | 7'-0" | | | OUTLET WINGS VERTICAL B.F. |
| D408 | X | 24 | 14'-7" | △ | X | OUTLET WINGS CORNER B.F. |
| D409 | X | 38 | 10'-6" | △ | | OUTLET WINGS VERTICAL F.F. |
| D410 | X | 26 | 18'-6" | | | OUTLET WINGS HORIZONTAL |
| D411 | X | 28 | 10'-3" | △ | | OUTLET WINGS HORIZONTAL |
| D512 | X | 4 | 19'-4" | | | OUTLET WINGS HORIZONTAL, TOP |
| D413 | X | 4 | 3'-5" | | | OUTLET BACKWALL |
| D414 | X | 4 | 5'-5" | | | OUTLET BACKWALL |
| D415 | X | 4 | 7'-5" | | | OUTLET BACKWALL |
| D416 | X | 4 | 8'-1" | | | OUTLET BACKWALL |
| D417 | X | 4 | 6'-1" | | | OUTLET BACKWALL |
| D418 | X | 4 | 4'-1" | | | OUTLET BACKWALL |
| D419 | X | 8 | 14'-8" | | | OUTLET BACKWALL |
| D420 | X | 4 | 16'-7" | | | OUTLET BACKWALL |
| | | | | | | |

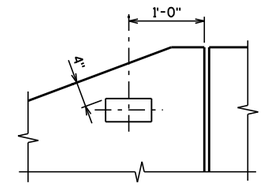
△ INDICATES BAR SERIES



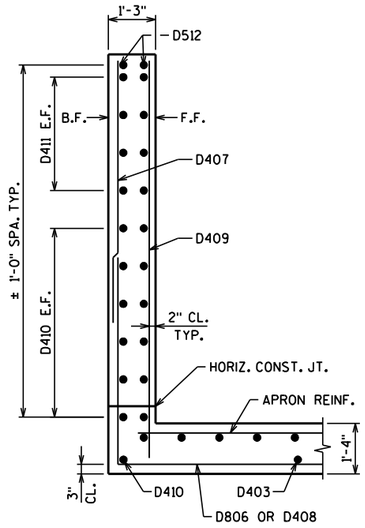

## BAR BEND DIAGRAMS

## BAR SERIES TABLE

| BAR NO. | NO. REQ'D. | LENGTH |
|---|---|---|
| D404 | 2 SERIES OF 4 | 2'-6" TO 13'-10" |
| D405 | 1 SERIES OF 17 | 17'-3" TO 25'-9" |
| D806 | 2 SERIES OF 19 | 12'-10" TO 17'-6" |
| D408 | 2 SERIES OF 12 | 13'-1" TO 16'-1" |
| D409 | 2 SERIES OF 19 | 6'-5" TO 14'-7" |
| D411 | 4 SERIES OF 7 | 3'-9" TO 16'-9" |
| | | |

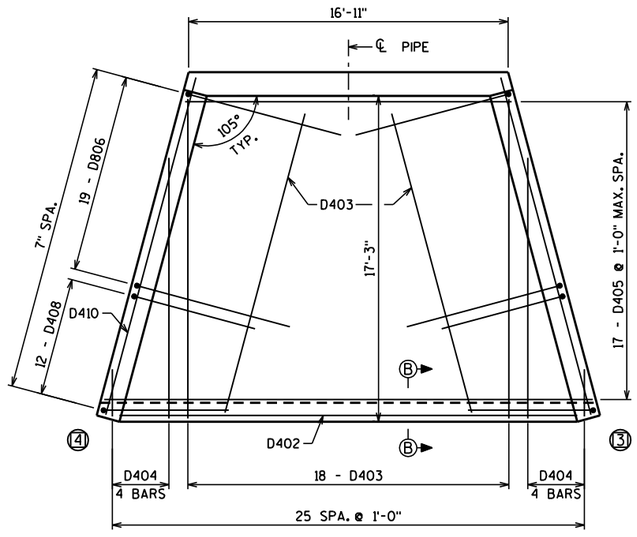



## NAME PLATE LOCATION WING 4

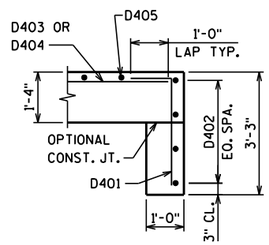



## SECTION A-A

(WING 3 SIMILAR)




## DOWNSTREAM APRON PLAN




## SECTION B-B

| NO. | DATE | REVISION | BY |
|---|---|---|---|
| | | | |

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE C-35-19

DRAWN BY BRE | PLANS CK'D. KRO

SOUTH APRON DETAILS

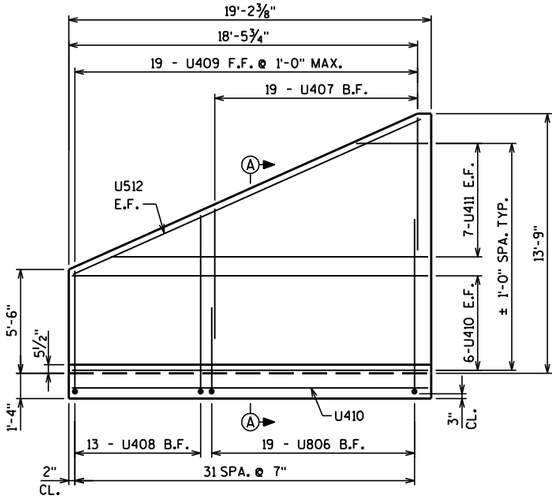
SHEET 2 OF 4

32

8

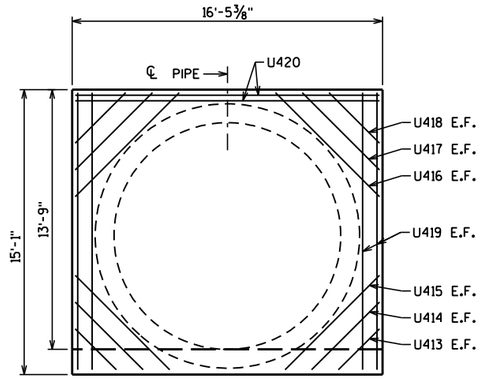
8

STATE PROJECT NUMBER
9000-05-60




## WING 1 ELEVATION

(WING 2 SIMILAR)




## BACKWALL FRONT FACE ELEVATION

## NOTES:

PLACE BARS AS SHOWN. SPACE AT 1'-0" ± UNLESS NOTED OTHERWISE.

(X) INDICATES WING NUMBER

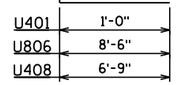
## BILL OF BARS

| BAR MARK | COAT | NO. REQ'D. | LENGTH | SERIES | BENT | LOCATION |
|---|---|---|---|---|---|---|
| U401 | | 34 | 3'-6" | | X | INLET CUTOFF WALL |
| U402 | | 4 | 33'-6" | | | INLET CUTOFF WALL |
| U403 | | 20 | 15'-8" | | | INLET APRON |
| U404 | | 16 | 8'-5" | △ | | INLET APRON |
| U405 | | 16 | 25'-11" | △ | | INLET APRON |
| U806 | X | 38 | 15'-2" | △ | X | INLET WINGS CORNER B.F. |
| U407 | X | 38 | 7'-0" | | | INLET WINGS VERTICAL B.F. |
| U408 | X | 26 | 14'-8" | △ | X | INLET WINGS CORNER B.F. |
| U409 | X | 38 | 10'-6" | △ | | INLET WINGS VERTICAL F.F. |
| U410 | X | 26 | 18'-10" | | | INLET WINGS HORIZONTAL |
| U411 | X | 28 | 10'-5" | △ | | INLET WINGS HORIZONTAL |
| U512 | X | 4 | 20'-1" | | | INLET WINGS HORIZONTAL, TOP |
| U413 | X | 4 | 3'-1" | | | INLET BACKWALL |
| U414 | X | 4 | 5'-1" | | | INLET BACKWALL |
| U415 | X | 4 | 7'-1" | | | INLET BACKWALL |
| U416 | X | 4 | 7'-9" | | | INLET BACKWALL |
| U417 | X | 4 | 5'-9" | | | INLET BACKWALL |
| U418 | X | 4 | 3'-9" | | | INLET BACKWALL |
| U419 | X | 8 | 14'-8" | | | INLET BACKWALL |
| U420 | X | 4 | 16'-1" | | | INLET BACKWALL |
| | | | | | | |

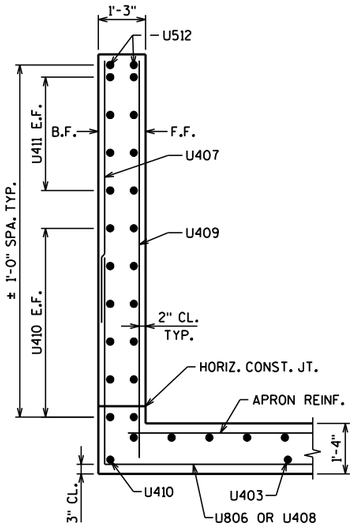
△ INDICATES BAR SERIES

## BAR SERIES TABLE

| BAR NO. | NO. REQ'D. | LENGTH |
|---|---|---|
| U404 | 2 SERIES OF 8 | 2'-4" TO 14'-6" |
| U405 | 1 SERIES OF 16 | 17'-3" TO 34'-7" |
| U806 | 2 SERIES OF 19 | 12'-10" TO 17'-6" |
| U408 | 2 SERIES OF 13 | 13'-1" TO 16'-3" |
| U409 | 2 SERIES OF 19 | 6'-5" TO 14'-7" |
| U411 | 4 SERIES OF 7 | 3'-8" TO 17'-2" |
| | | |

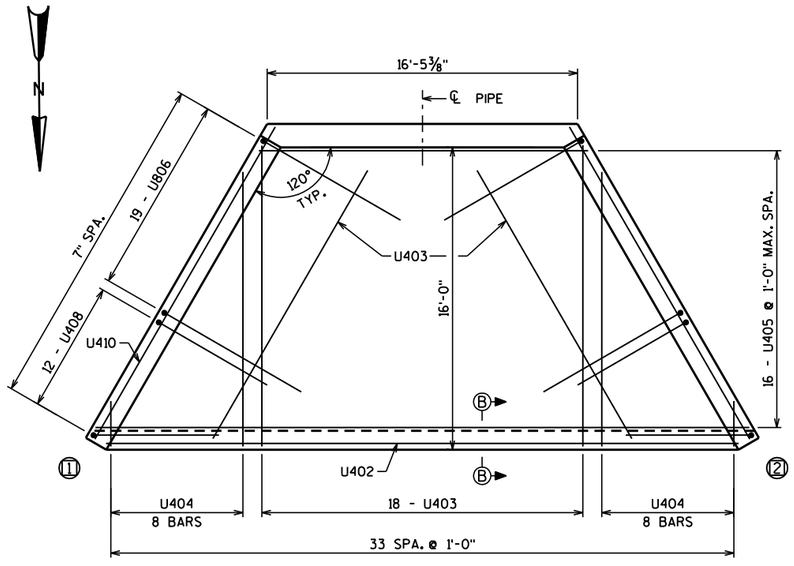



## BAR BEND DIAGRAMS

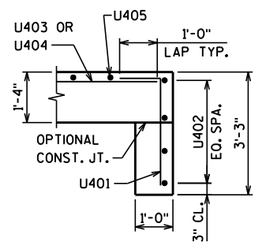



## SECTION A-A

(WING 2 SIMILAR)




## UPSTREAM APRON PLAN




## SECTION B-B

| NO. | DATE | REVISION | BY |
|---|---|---|---|
| | | | |

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE C-35-19

DRAWN BY BRE | PLANS CK'D. KRO

NORTH APRON DETAILS

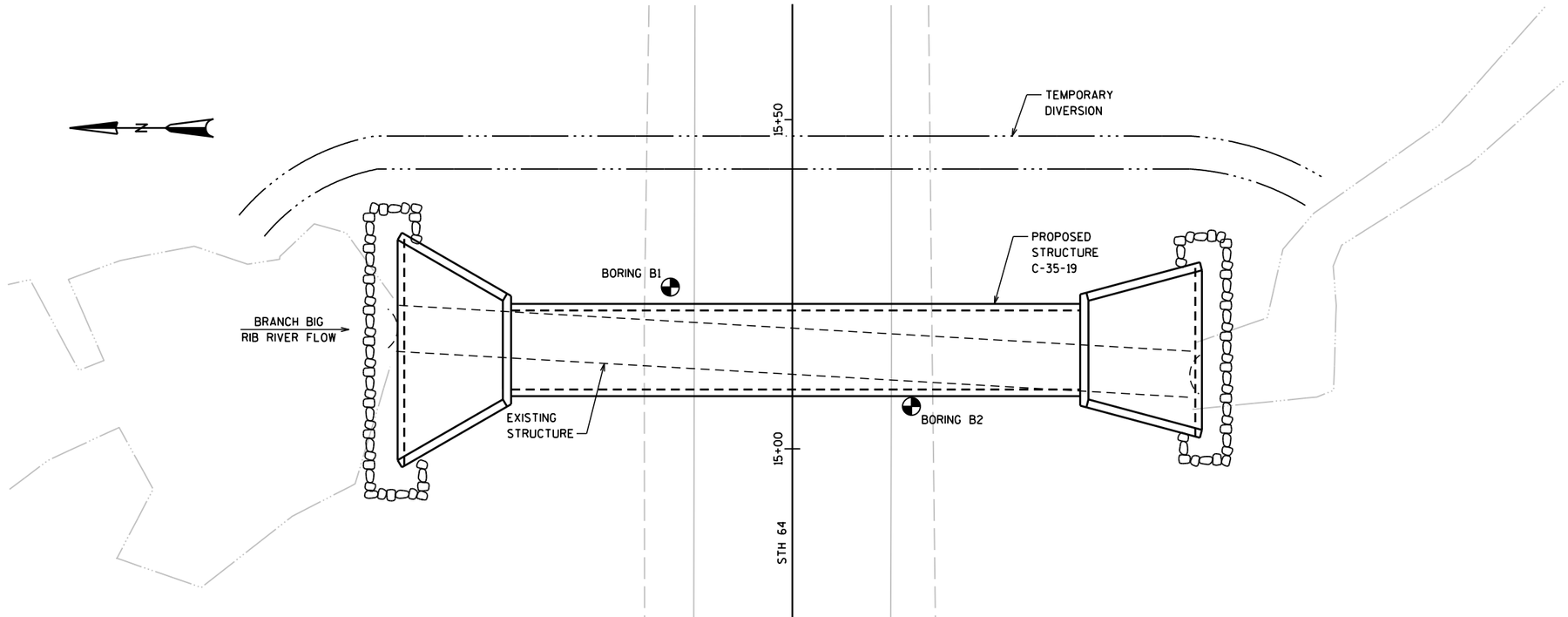
SHEET 3 OF 4

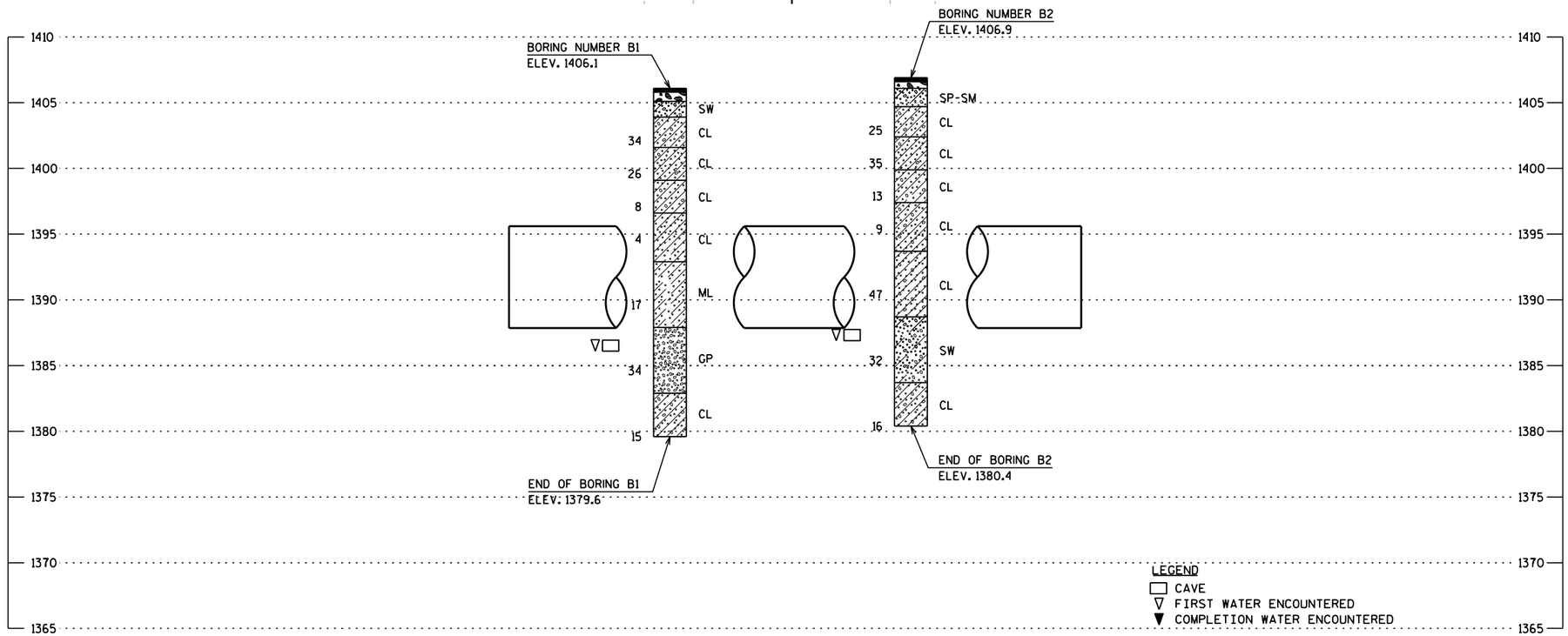
33

8

8

| STATE PROJECT NUMBER |
|---|
| 9000-05-60 |



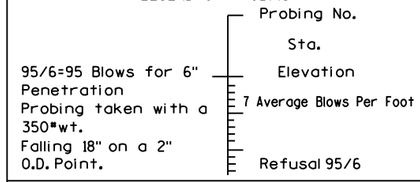

ABBREVIATIONS

F — Fine  M — Medium  C — Coarse
Ws — Weathered  So — Sound

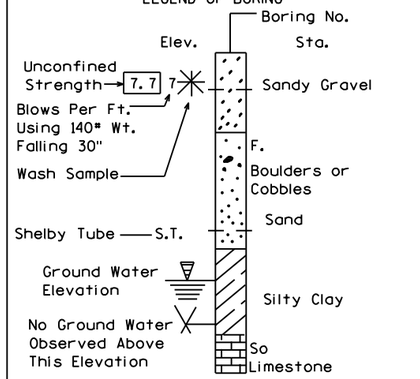
MATERIAL SYMBOLS

Topsoil, Silt, Sandstone
Sand, Peat, Limestone
Gravel, Clay, Igneous Rock

LEGEND OF PROBING




LEGEND OF BORING




Unless otherwise specified, the blows per foot at the locations indicated are based on driving a O.D.x1.4" I.D. split spoon sampler with a 140# hammer having a free fall of 30". The blow count is taken in undisturbed soil immediately below a cased or open hole eliminating side friction on the drive pipe.

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

To obtain relative data concerning the character of material in and upon which the foundation might be built, borings and/or soundings were made at points approximately as indicated on this drawing. The data presented herein represents the findings of the subsurface explorations made. However, because the depths investigated are limited and the area of the borings and/or soundings is very small in relation to the entire area, the Division of Highways does not warrant conditions below the depths investigated or that the classification of material encountered in these investigations is necessarily typical of the entire site.

| NO. | DATE | REVISION | BY |
|---|---|---|---|

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE C-35-19

DRAWN BY BRE  PLANS CK'D. KRO

SUBSURFACE EXPLORATION

SHEET 4 OF 4

34

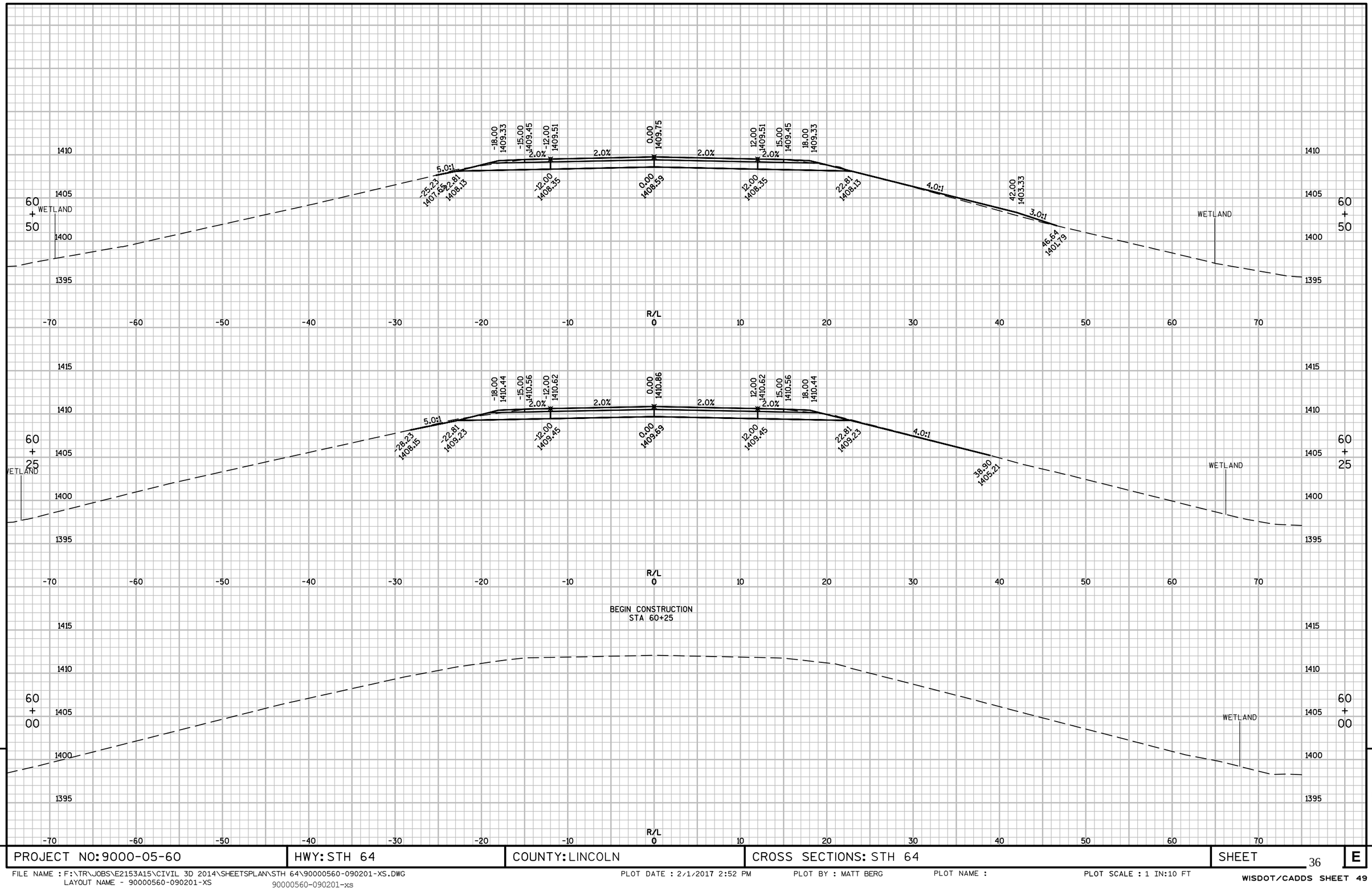
8

8

## STH 64 EARTHWORK - CATEGORY 0010

| STATION | INCREMENTAL AREA (SF) | | | | INCREMENTAL VOL (CY) (UNADJUSTED) | | | | CUMULATIVE VOL (CY) | | | MASS ORDINATE |
|---|---|---|---|---|---|---|---|---|---|---|---|---|
| | CUT | FILL | MARSH EXC | PIPE EXC | CUT | FILL | MARSH EXC | PIPE EXC | CUT + PIPE EXC | EXPANDED FILL | EXPANDED MARSH BACKFILL | |
| | NOTE 1 | | | NOTE 3 | | | | | FACTOR 1.00 | FACTOR 1.25 | FACTOR 1.50 | NOTE 2 |
| START ABRUPTLY | | | | | | | | | | | | |
| 60+25 | 47.0 | 0.0 | 0.0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| 60+50 | 47.0 | 3.7 | 0.0 | 0 | 44 | 2 | 0 | 0 | 44 | 2 | 0 | 41 |
| 60+55 | 46.4 | 5.3 | 0.0 | 0 | 9 | 1 | 0 | 0 | 52 | 3 | 0 | 49 |
| 60+65 | 45.1 | 8.8 | 0.0 | 65 | 17 | 3 | 0 | 12 | 81 | 6 | 0 | 75 |
| 60+75 | 45.3 | 24.4 | 0.0 | 133 | 17 | 6 | 0 | 37 | 135 | 14 | 0 | 120 |
| 60+84 | 45.4 | 36.0 | 10.0 | 196 | 15 | 10 | 2 | 55 | 205 | 27 | 3 | 178 |
| 61+00 | 44.5 | 87.0 | 33.4 | 591 | 27 | 36 | 13 | 233 | 464 | 72 | 22 | 392 |
| 61+05 | 43.7 | 73.3 | 40.0 | 723 | 8 | 15 | 7 | 122 | 594 | 91 | 32 | 503 |
| 61+15 | 41.4 | 37.6 | 62.4 | NOTE 4 | 16 | 21 | 19 | 0 | 610 | 116 | 60 | 493 |
| 61+25 | 42.9 | 78.6 | 57.7 | 688 | 16 | 22 | 22 | 0 | 625 | 143 | 94 | 482 |
| 61+43 | 47.2 | 41.1 | 35.0 | 188 | 30 | 40 | 31 | 292 | 948 | 193 | 140 | 754 |
| 61+50 | 48.6 | 35.8 | 28.4 | 144 | 12 | 10 | 8 | 43 | 1003 | 206 | 152 | 797 |
| 61+65 | 50.0 | 24.7 | 0.0 | 56 | 27 | 17 | 8 | 56 | 1086 | 227 | 164 | 859 |
| 61+75 | 51.3 | 24.4 | 0.0 | 0 | 19 | 9 | 0 | 10 | 1115 | 238 | 164 | 877 |
| 62+00 | 50.8 | 21.7 | 0.0 | 0 | 47 | 21 | 0 | 0 | 1162 | 265 | 164 | 898 |
| 62+25 | 44.9 | 0.2 | 0.0 | 0 | 44 | 10 | 0 | 0 | 1207 | 277 | 164 | 929 |
| END ABRUPTLY | | | | | | | | | | | | |

NOTES: 1) ALL EXCAVATED ASPHALT MATERIAL ASSUMED AVAILABLE AS FILL.
2) MASS ORDINATE = EXCAVATION COMMON - EXPANDED FILL
3) PIPE EXC. IS EXCAVATION COMMON REQUIRED FOR PIPE INSTALLATION. SEE CULVERT PIPE EXCAVATION AND INSTALLATION DETAIL.
4) END PIPE EXC ABRUPTLY STA 61+05 AND BEGIN PIPE EXC ABRUPTLY 61+25.

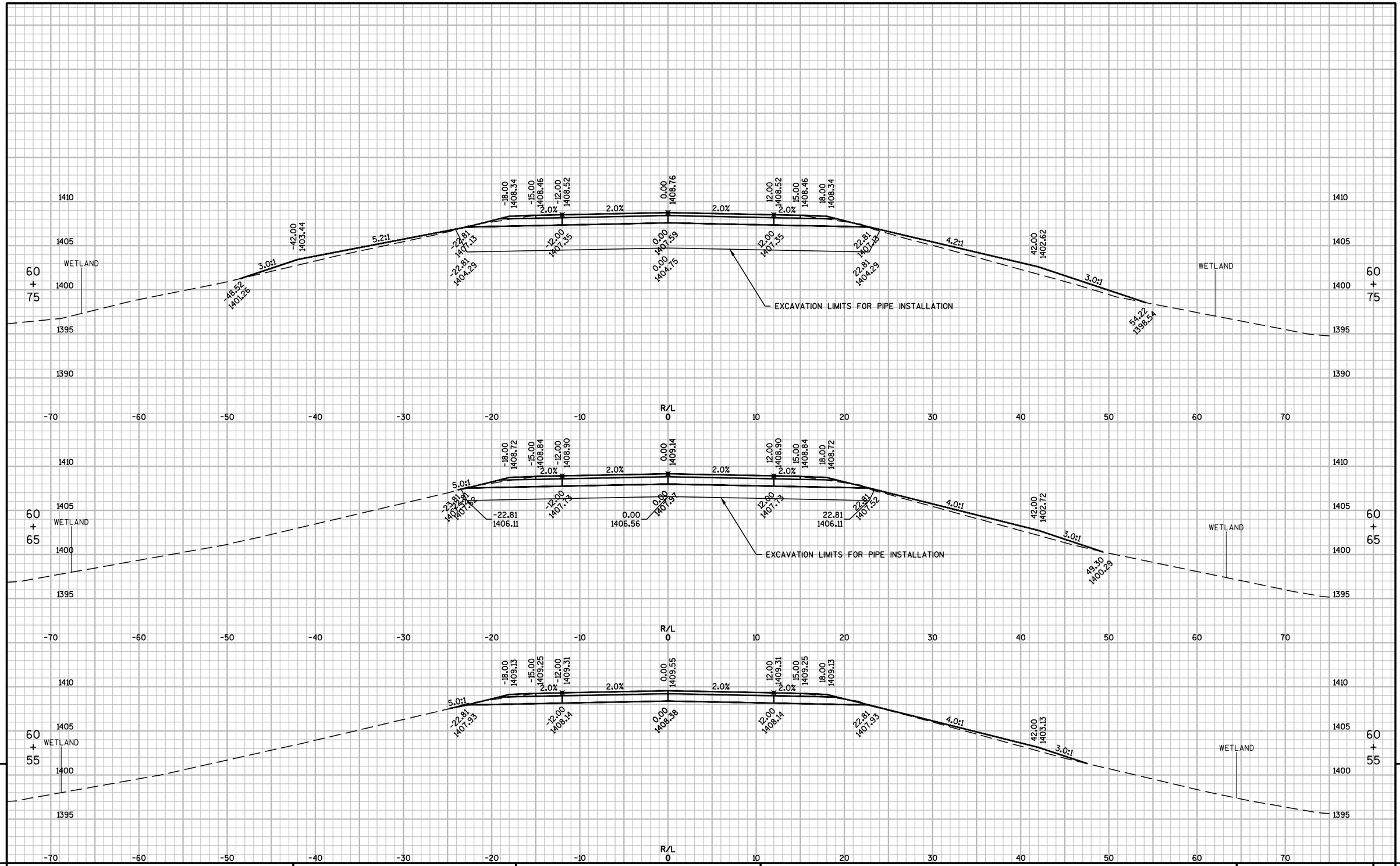



9

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | CROSS SECTIONS: STH 64 | SHEET | E |
|---|---|---|---|---|---|



FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLAN\STH 64\90000560-090201-XS.DWG
LAYOUT NAME - 90000560-090201-XS
90000560-090201-xs
PLOT DATE : 2/1/2017 2:52 PM PLOT BY : MATT BERG PLOT NAME : PLOT SCALE : 1 IN:10 FT




| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | CROSS SECTIONS: STH 64 | SHEET 37 | E |
|---|---|---|---|---|---|

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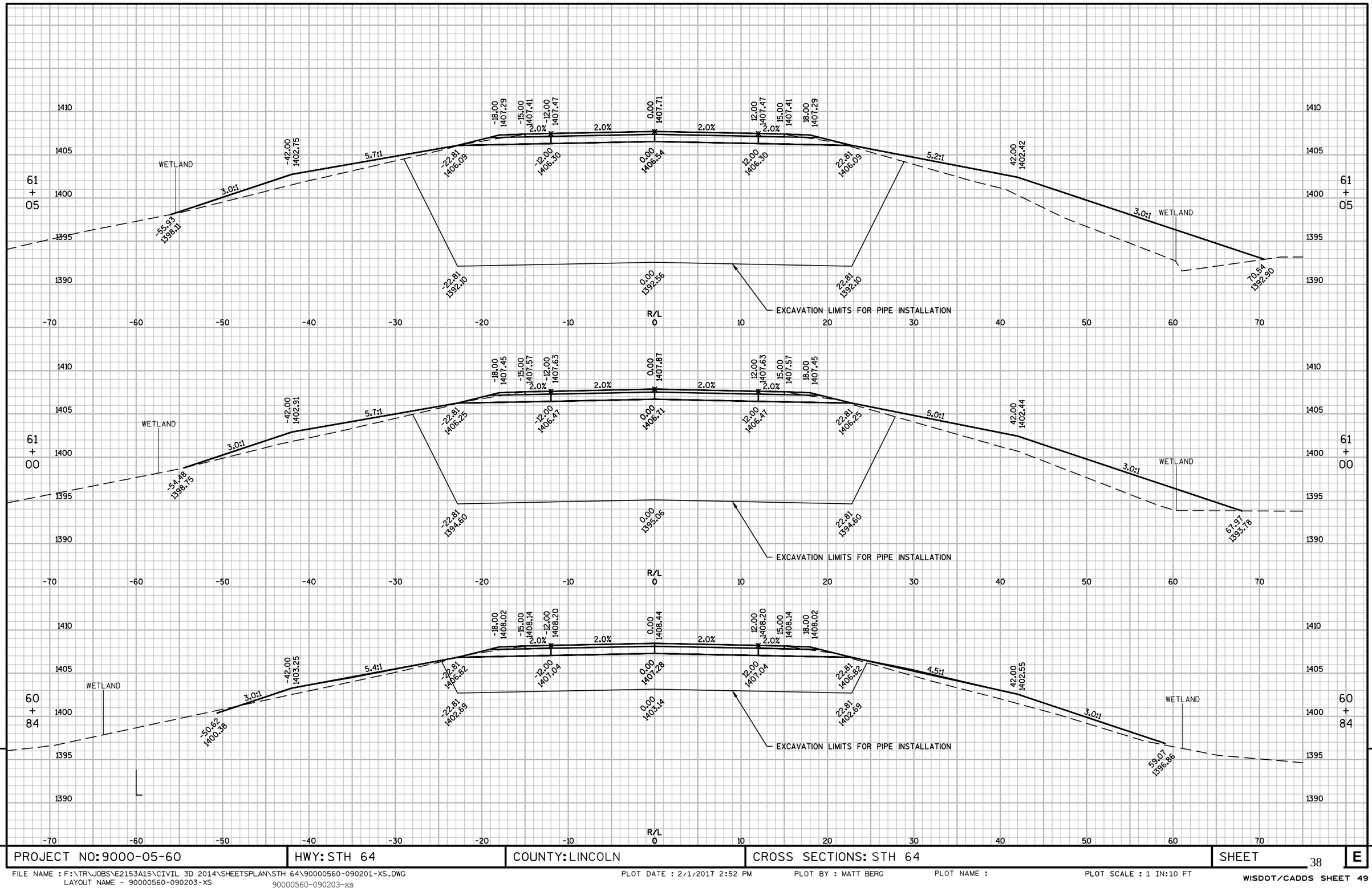
LAYOUT NAME - 90000560-090202-XS
90000560-090202-xs

PLOT DATE : 2/1/2017 2:52 PM PLOT BY : MATT BERG PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49




9

9

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | CROSS SECTIONS: STH 64 | SHEET 38 | E |
|---|---|---|---|---|---|

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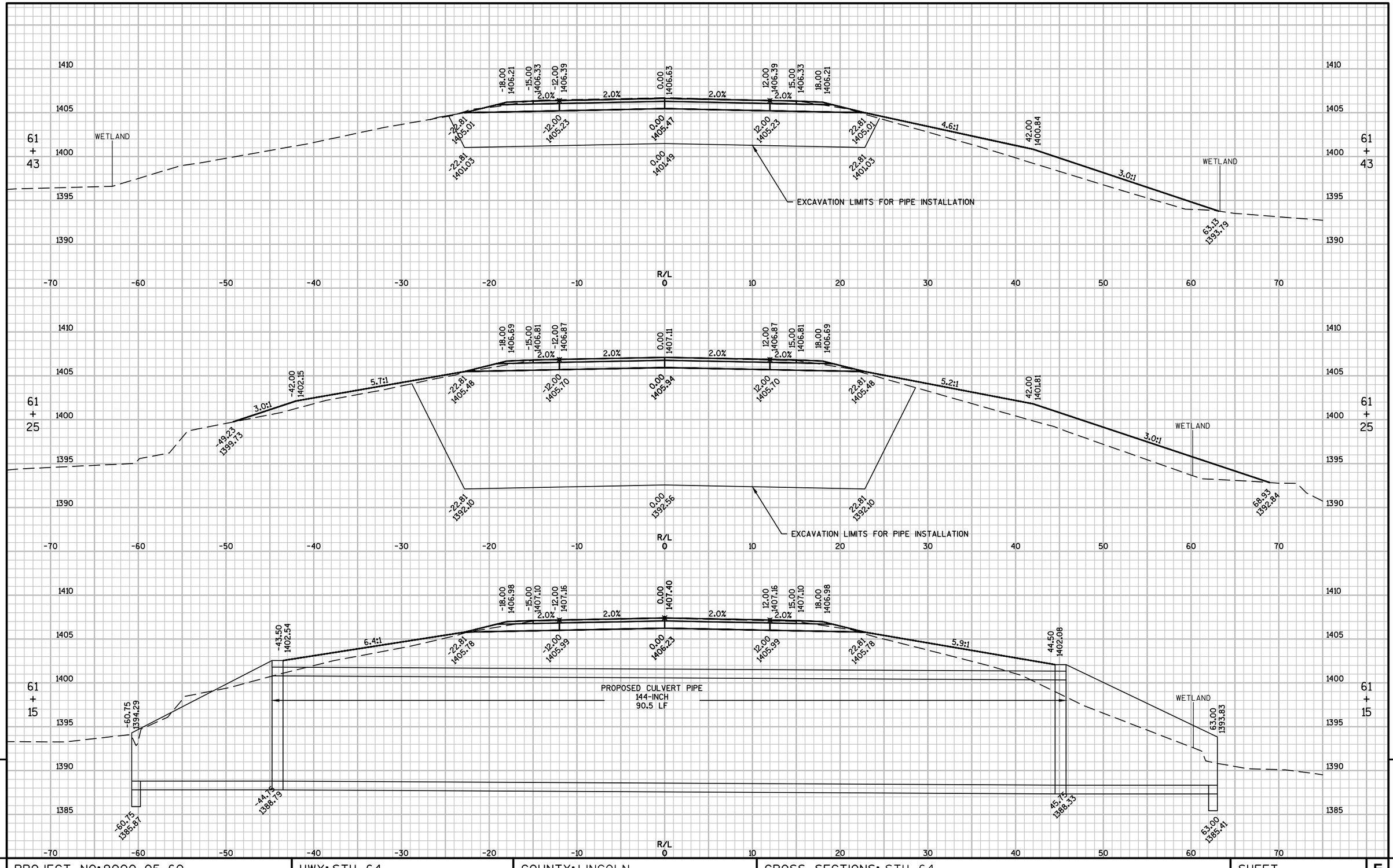
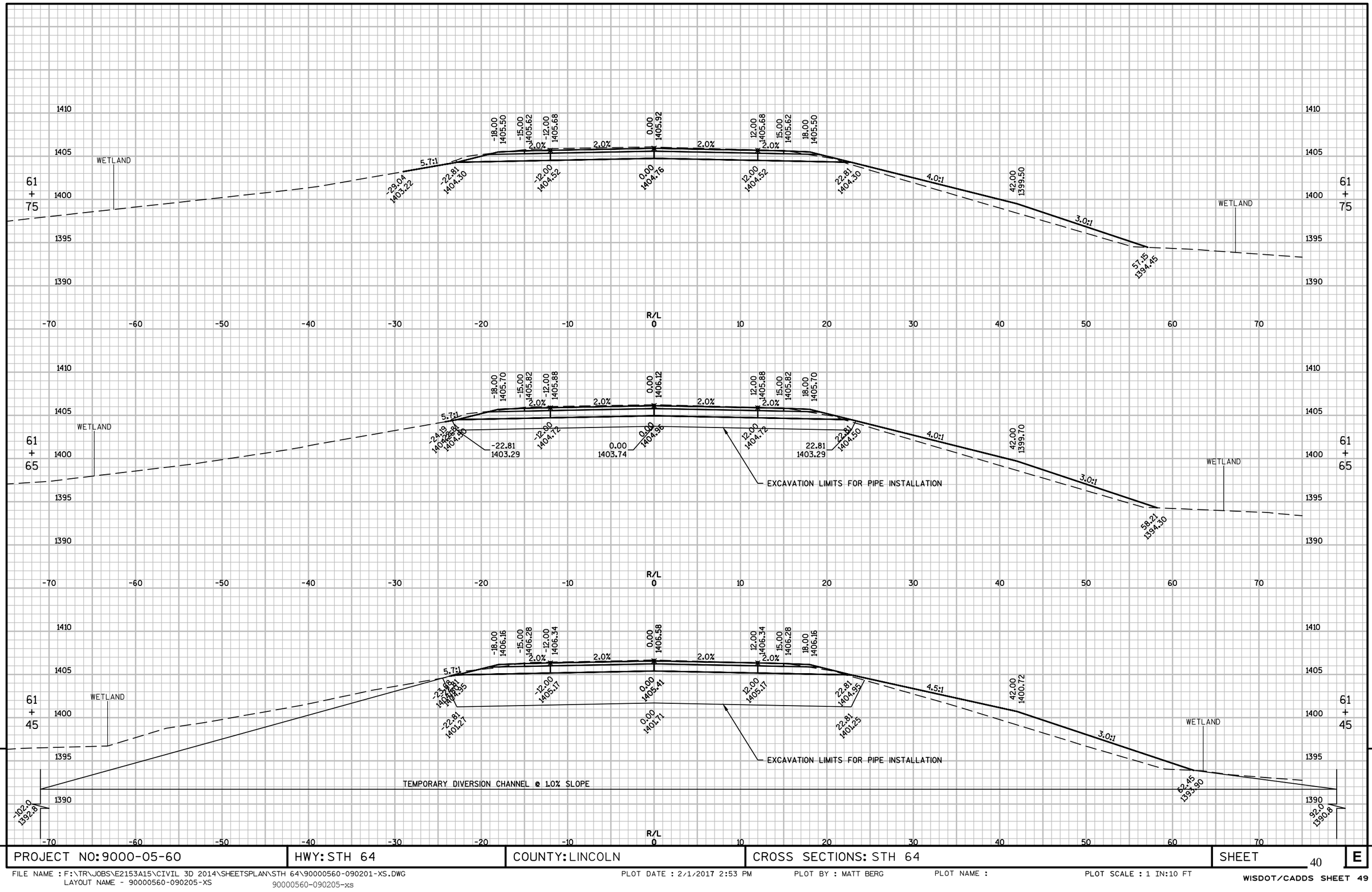
LAYOUT NAME - 90000560-090203-XS
90000560-090203-xs
PLOT DATE : 2/1/2017 2:52 PM
PLOT BY : MATT BERG
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

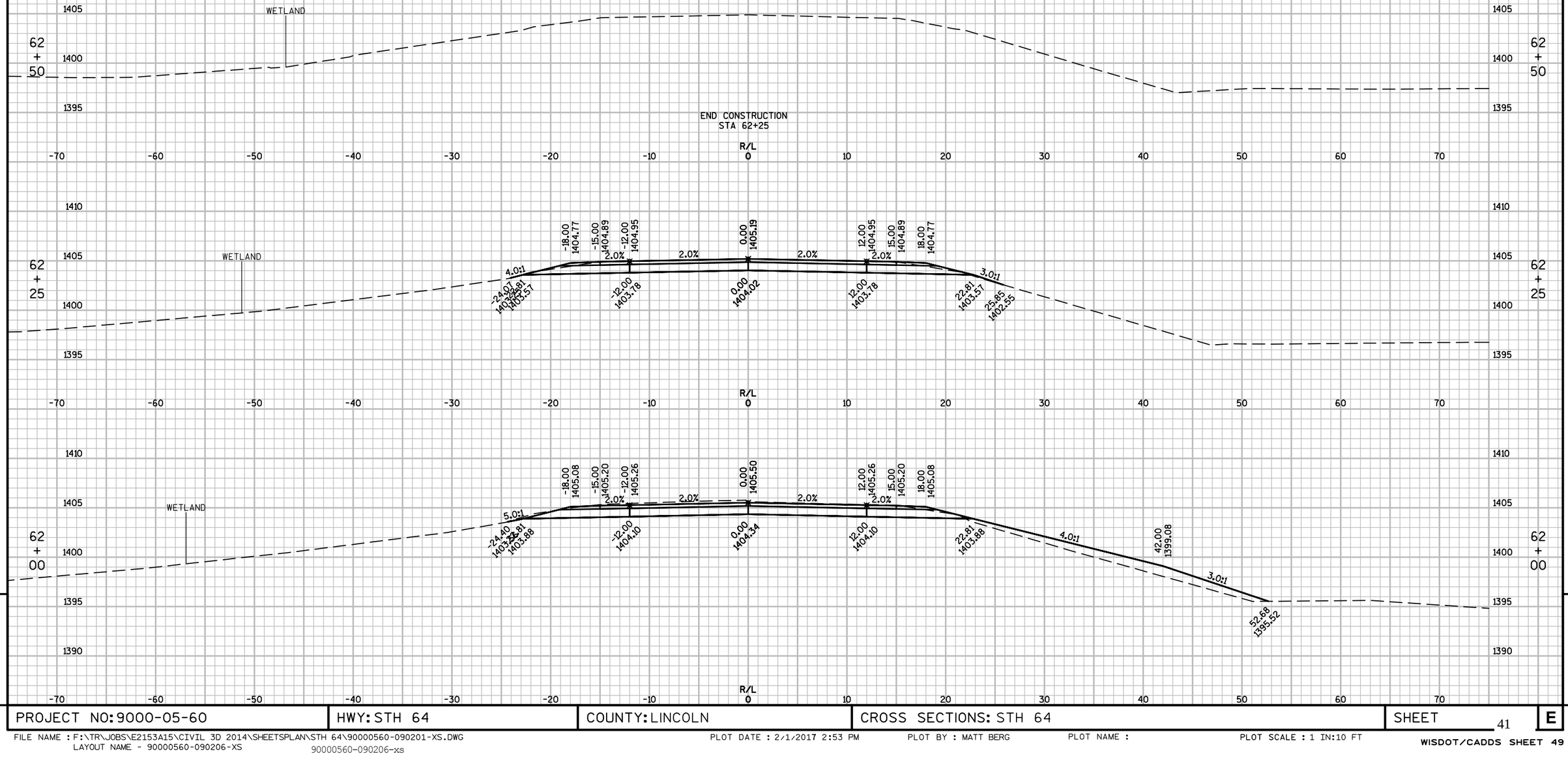



| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | CROSS SECTIONS: STH 64 | SHEET 39 | E |
|---|---|---|---|---|---|




| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | CROSS SECTIONS: STH 64 | SHEET 40 | E |
|---|---|---|---|---|---|

FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLAN\STH 64\90000560-090201-XS.DWG
LAYOUT NAME - 90000560-090205-XS
90000560-090205-xs
PLOT DATE : 2/1/2017 2:53 PM
PLOT BY : MATT BERG
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49




9

9

| PROJECT NO: 9000-05-60 | HWY: STH 64 | COUNTY: LINCOLN | CROSS SECTIONS: STH 64 | SHEET 41 | E |
|---|---|---|---|---|---|

FILE NAME : F:\TR\JOBS\E2153A15\CIVIL 3D 2014\SHEETSPLAN\STH 64\90000560-090201-XS.DWG
LAYOUT NAME - 90000560-090206-XS
90000560-090206-xs

PLOT DATE : 2/1/2017 2:53 PM PLOT BY : MATT BERG PLOT NAME : PLOT SCALE : 1 IN:10 FT

**Wisconsin Department of Transportation**

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