WOODED OR SHRUB AREA

**JUNE 2017** 

ORDER OF SHEETS

Section No. 1 Section No. 2 Typical Sections and Details Section No. 3 Estimate of Quantities Section No. 3 Miscellaneous Quantitles Right of Way Plat Plan and Profile

Section No. 5 Standard Detail Drawings Section No. 6

Sign Plates

Computer Earthwork Data Section No. 9 Cross Sections

TOTAL SHEETS = 134

Section No. 7

# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

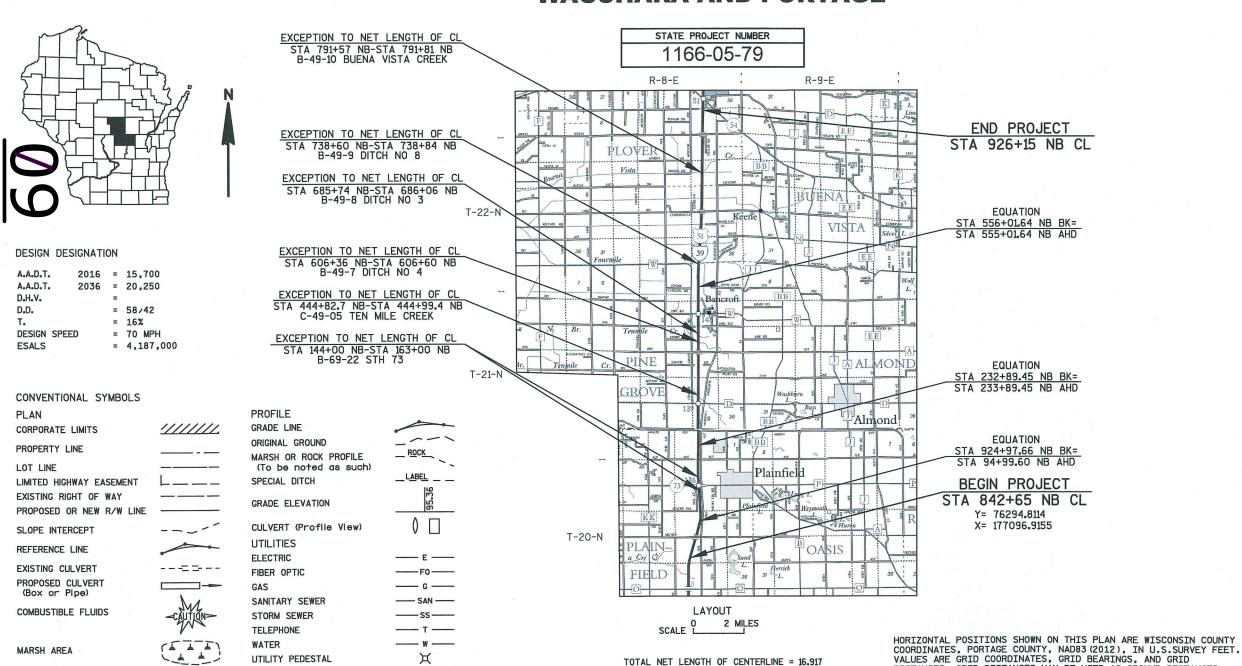
PLAN OF PROPOSED IMPROVEMENT

# **PLAINFIELD - STEVENS POINT**

CTH O TO STH 54, NB

# **IH 39**

# **WAUSHARA AND PORTAGE**



STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT

CONTRACT

1

**PROJECT** 

WISC 2017320

STATE PROJECT

1166-05-79

PREPARED BY NC REGION Surveyor ELIZABETH COLLINS Designer CHERYL SIMON NICHOLE LYSNE

APPROVED FOR THE DEPARTMENT

POWER POLE

TELEPHONE POLE

Ø

ELEVATIONS ARE REFERENCED TO NAVD 88

DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

#### **GENERAL NOTES**

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

PURSUANT TO CHAPTER 59 OF THE WISCONSIN STATUTES, THE CONTRACTOR SHALL CAREFULLY MAKE A SEARCH FOR EVIDENCE OF A LANDMARK IN ALL AREAS WHERE SUCH A LANDMARK MAY EXIST.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

Dial or (800)242-8511
www.DiggersHotline.com

#### UTILITIES

RICK PODOLAK
AT&T WISCONSIN-COMMUNICATION LINE
4TH FLOOR
304 SOUTH DEWEY STREET
EAU CLAIRE, WI 54701
(715) 839-5565
rp4514@att.com

JON TRZESNIAK
ADAMS-COLUMBIA ELECTRIC COOPERATIVE-ELECTRICITY
401 EAST LAKE STREET
PO BOX 70
FRIENDSHIP, WI 53934
(800) 831-8629
<a href="mailto:jttzesniak@acecwi.com">jttzesniak@acecwi.com</a>

RONALD BOWDEN
UNION TELEPHONE COMPANY-COMMUNICATION LINE
100 WEST NORTH STREET
PLAINFIELD, WI 54966-0096
(715) 335-6301
rbowden@uniontel.net

DON LUTZOW
WISCONSIN PUBLIC SERVICE CORPORATION-ELECRICITY
17000 SHERMAN STREET
WAUSAU, WI 54402
(715) 848-7487
dalutzow@wisconsinpublicservice.com

MARK VILLARS
ALLIANT ENERGY-ELECTRICITY
880 NORTH WISCONSIN STREET
BERLIN, WI 54923
(920) 361-5652
markvillars@alliantenergy.com

SEAN MANTEI KOCH PIPELINE COMPANY LP-GAS/PETROLEUM 2267 CTH HH JUNCTION CITY, WI 54443 (715) 498-6633 Sean.Mantei@kochpipeline.com

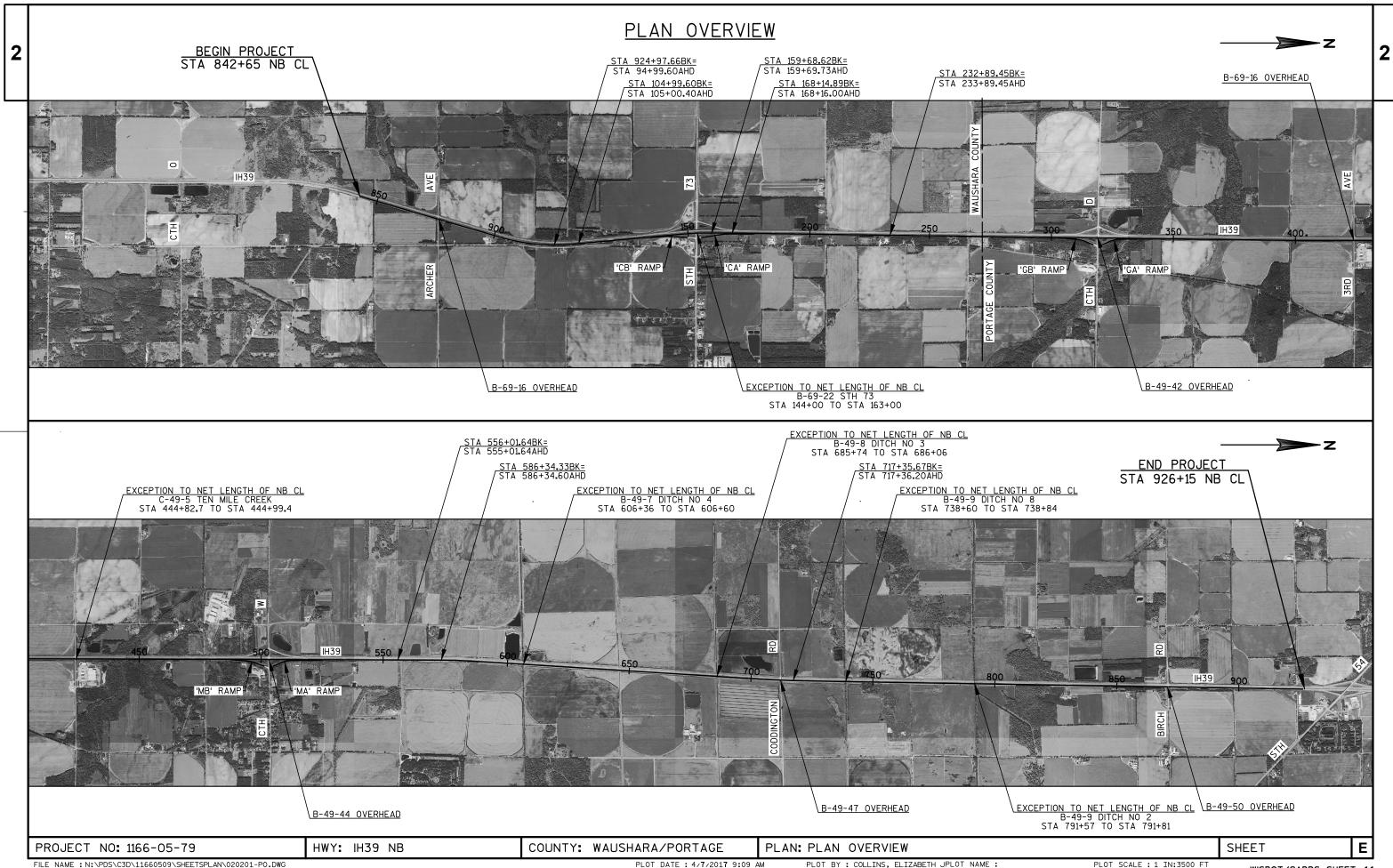
Send all WE Energies Correspondence to: LATROY BRUMFIELD WE ENERGIES-GAS/PETROLEUM 333 WEST EVERETT STREET, A299 MILWAUKEE, WI 53203 (414) 221-5617 latroy.brumfield@we-energies.com

RAY BOLTON (FIELD CONTACT)
WE ENERGIES-GAS/PETROLEUM
1921 8TH STREET SOUTH
WISCONSIN RAPIDS, WI 54494
(715) 630-5900
raymond.bolton@we-energies.com

MATTHEW CURTIN
WISCONSIN PUBLIC SERVICE CORPORATION-GAS/PETROLEUM
2001 PLOVER ROAD
PLOVER, WI 54467-3905
(715) 345-7582
mjcurtin@wisconsinpublicservice.com

RUDI RUDIGER
CHARTER COMMUNICATIONS - COMMUNICATION LINE
5024 HEFFRON STREET
STEVENS POINT, WI 54481
rudi.rudiger@charter.com

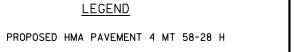
PROJECT NO: 1166-05-79 HWY: IH39 COUNTY: WAUSHARA/PORTAGE GENERAL NOTES SHEET: **E** 



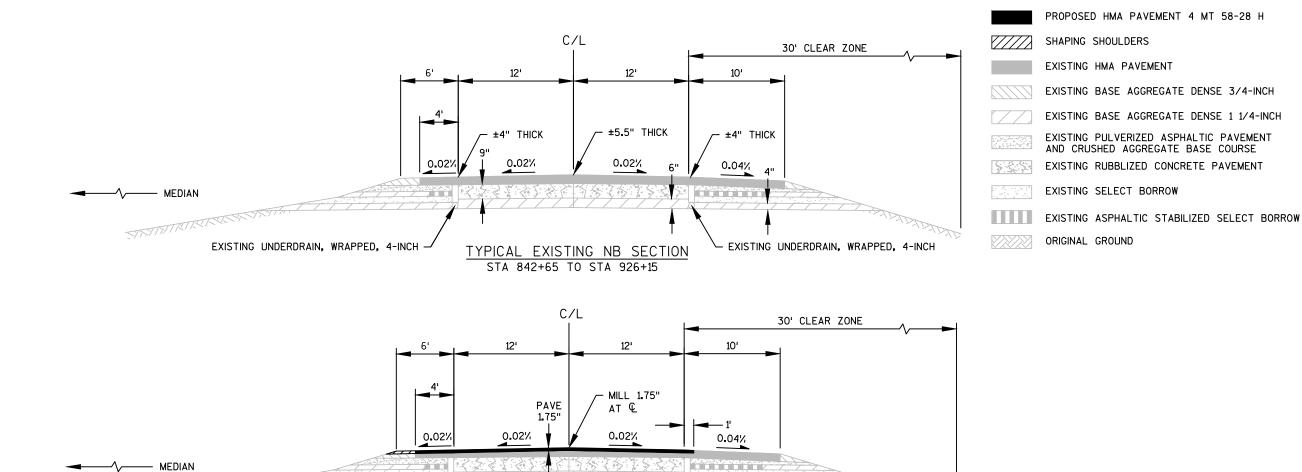
FILE NAME : N:\PDS\C3D\11660509\SHEETSPLAN\020201-P0.DWG

PLOT BY: COLLINS, ELIZABETH JPLOT NAME:

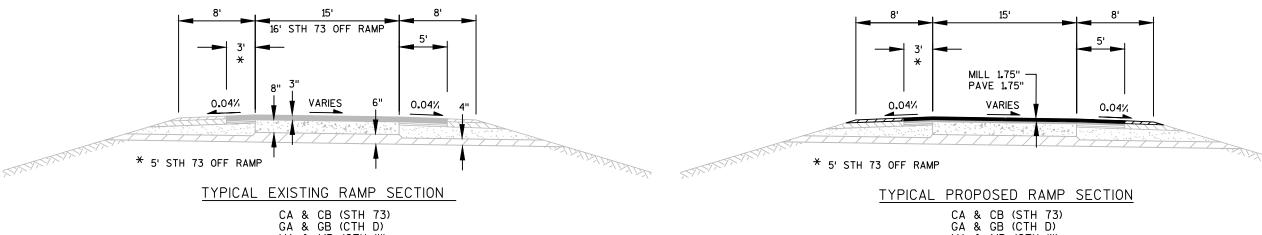
PLOT SCALE : 1 IN:3500 FT



LEGEND



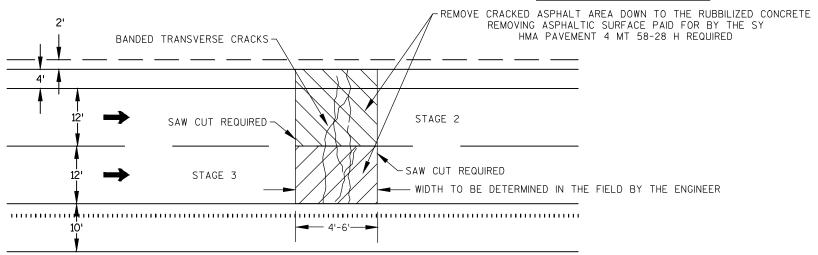
#### TYPICAL PROPOSED NB SECTION STA 842+65 TO STA 926+15



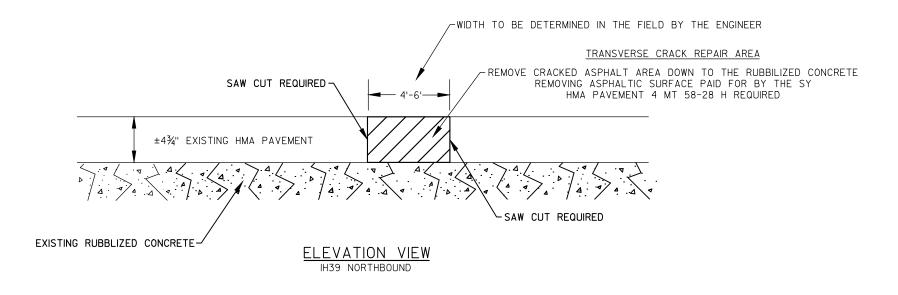
CA & CB (STH 73) GA & GB (CTH D) MA & MB (CTH W) MA & MB (CTH W)

E

#### TRANSVERSE CRACK REPAIR AREA

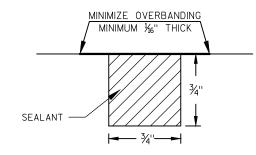


# PLAN VIEW IH39 NORTHBOUND



# TRANSVERSE CRACK REPAIR DETAIL

WORK TO BE DONE PRIOR TO MILLING LANE



## ROUT AND SEAL DETAIL

ROUT AND SEAL ALL CENTERLINE, LONGITUDINAL, AND TRANSVERSE CRACKS HAVING A WIDTH LESS THAN OR EQUAL TO ¾"

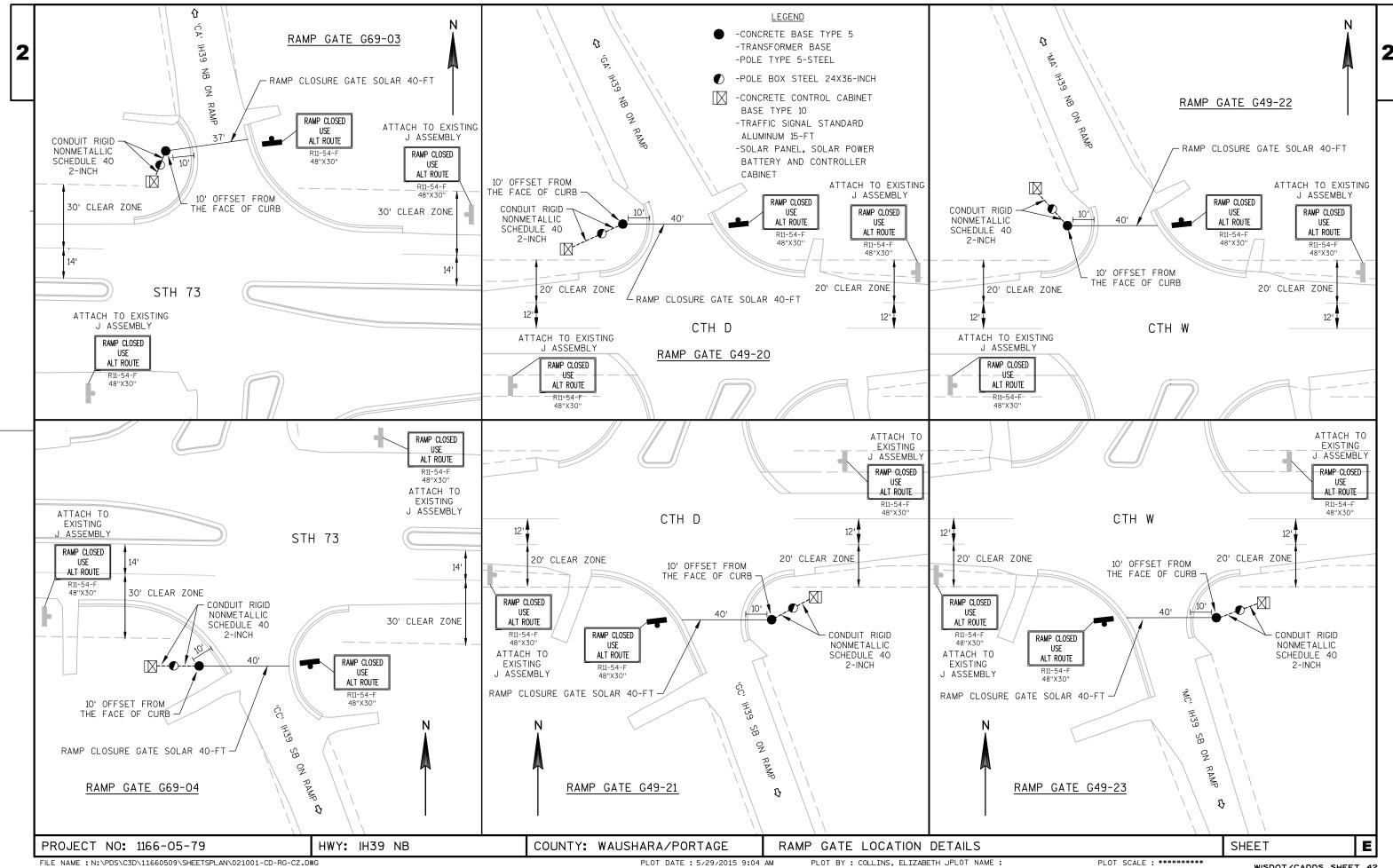
PROJECT NO: 1166-05-79 HWY: IH39 NB COUNTY: WAUSHARA/PORTAGE CONSTRUCTION DETAIL

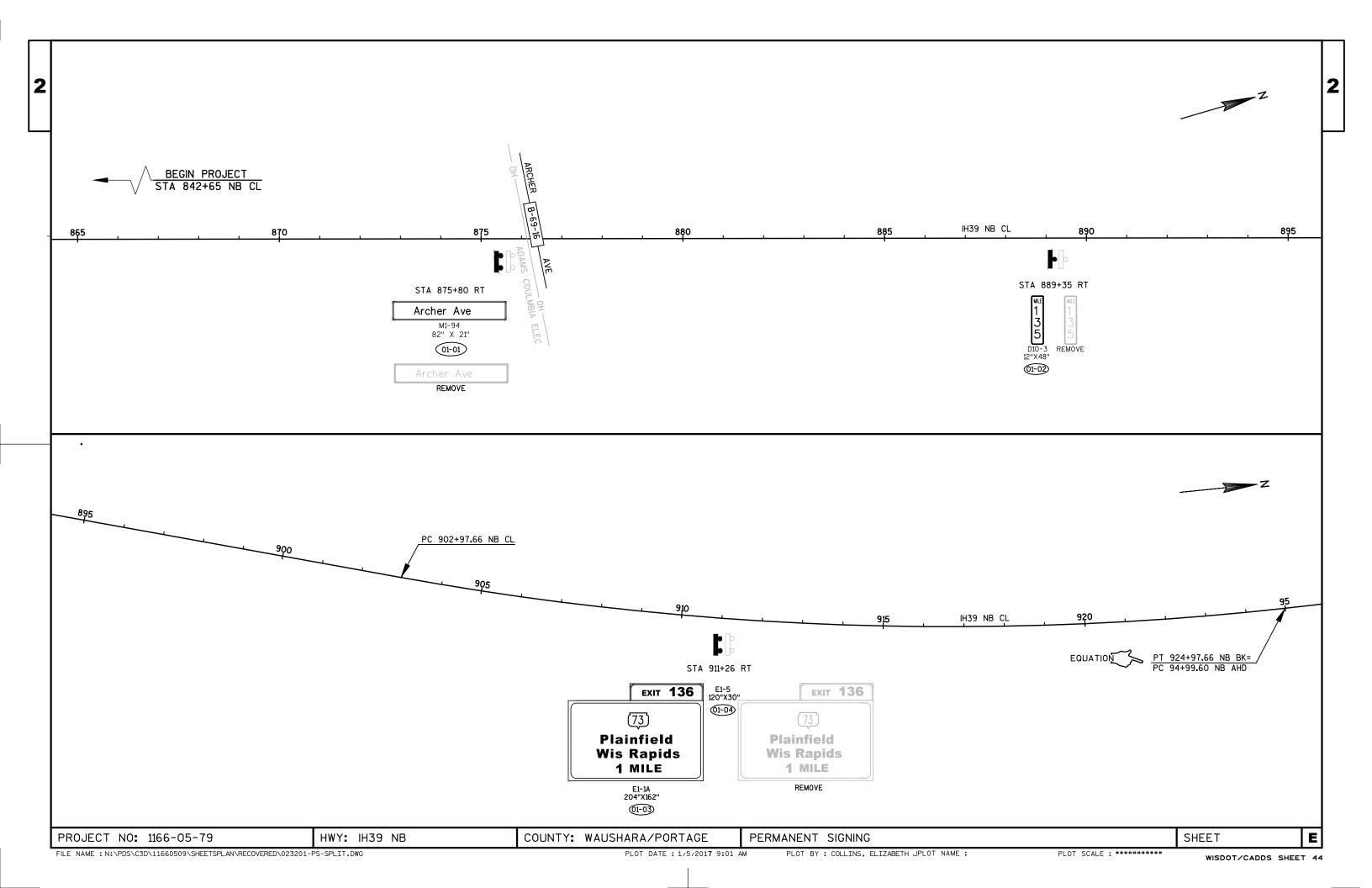
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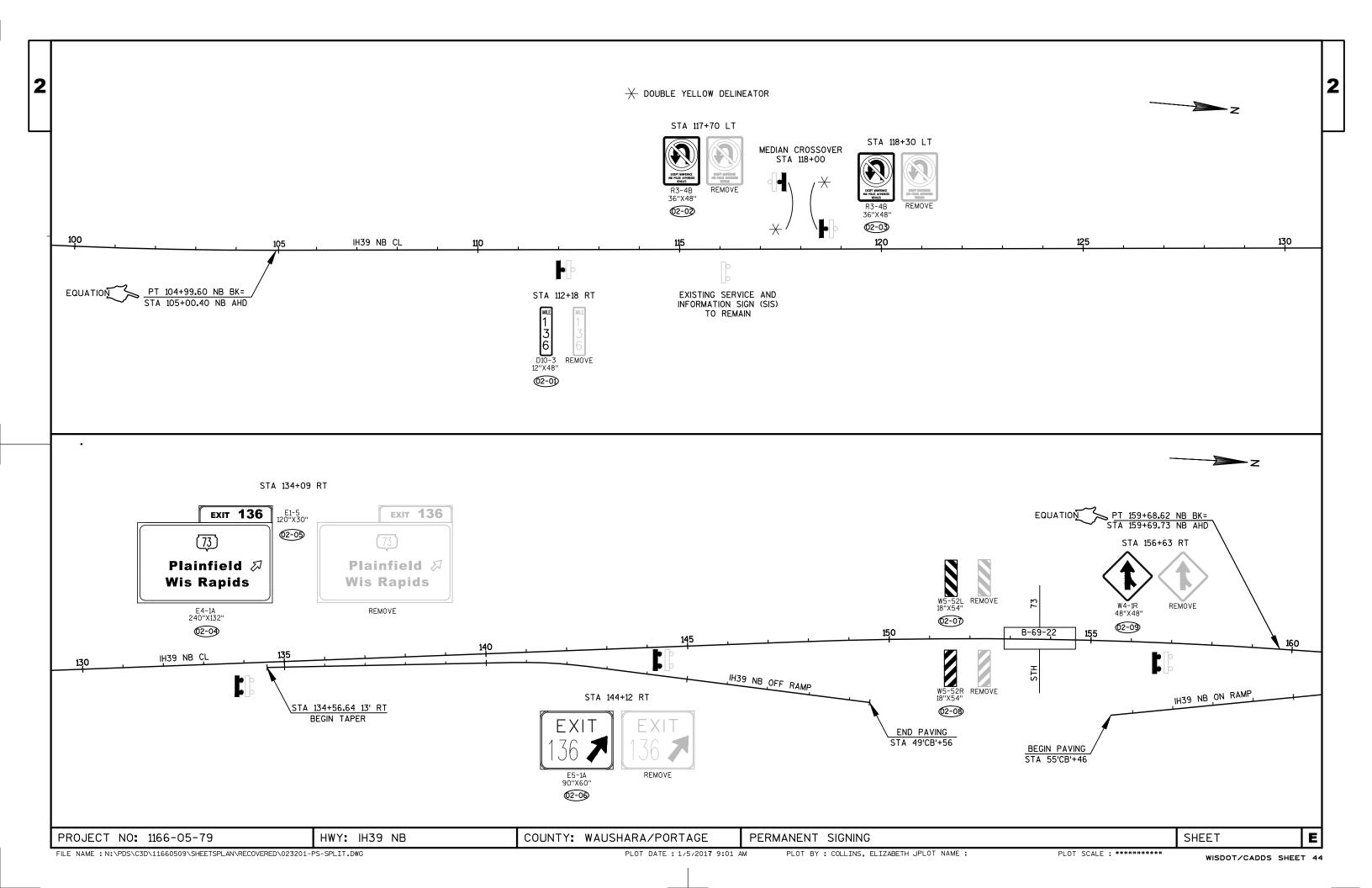
PLOT SCALE : 1 IN:200 FT

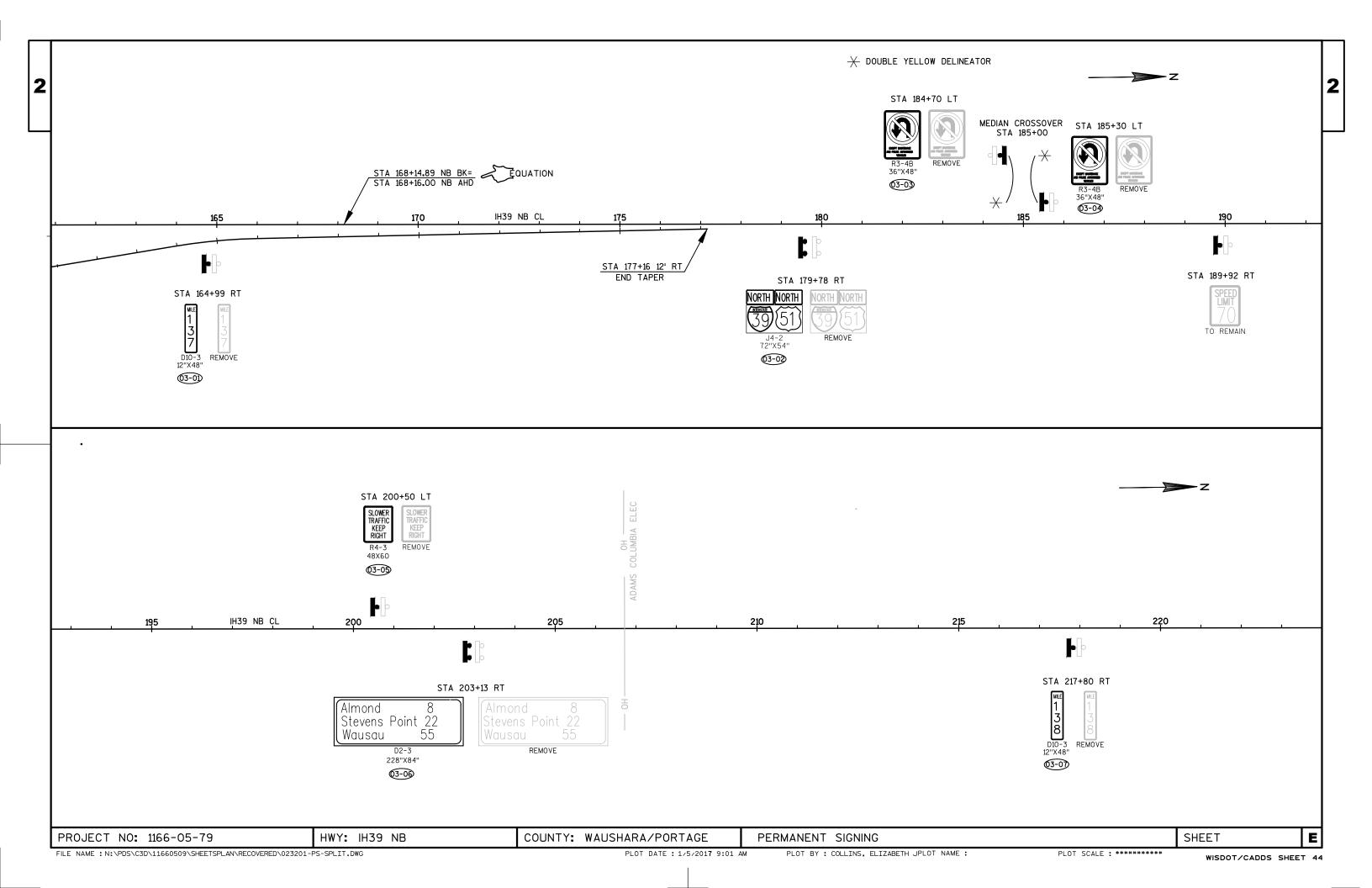
SHEET

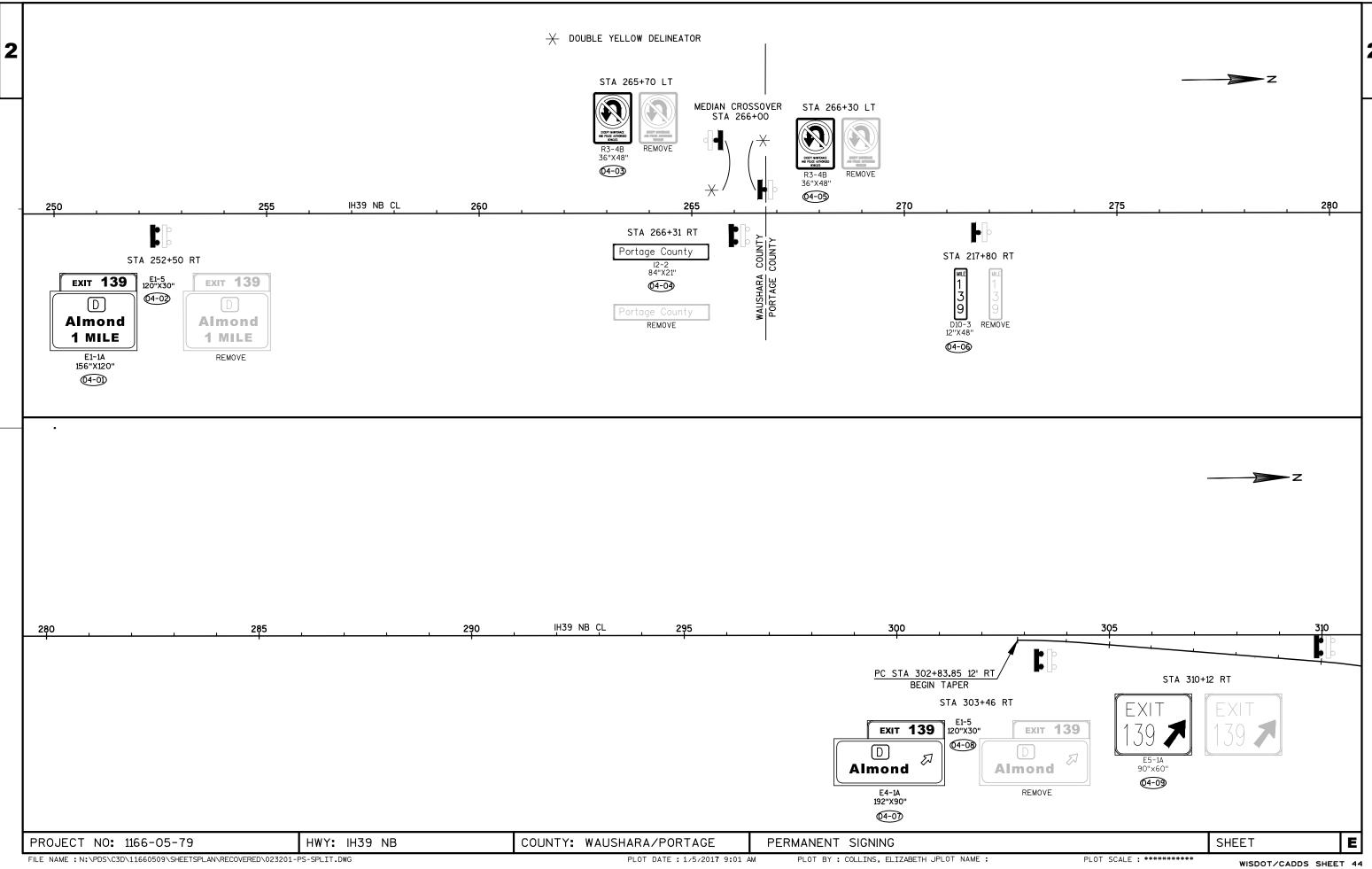
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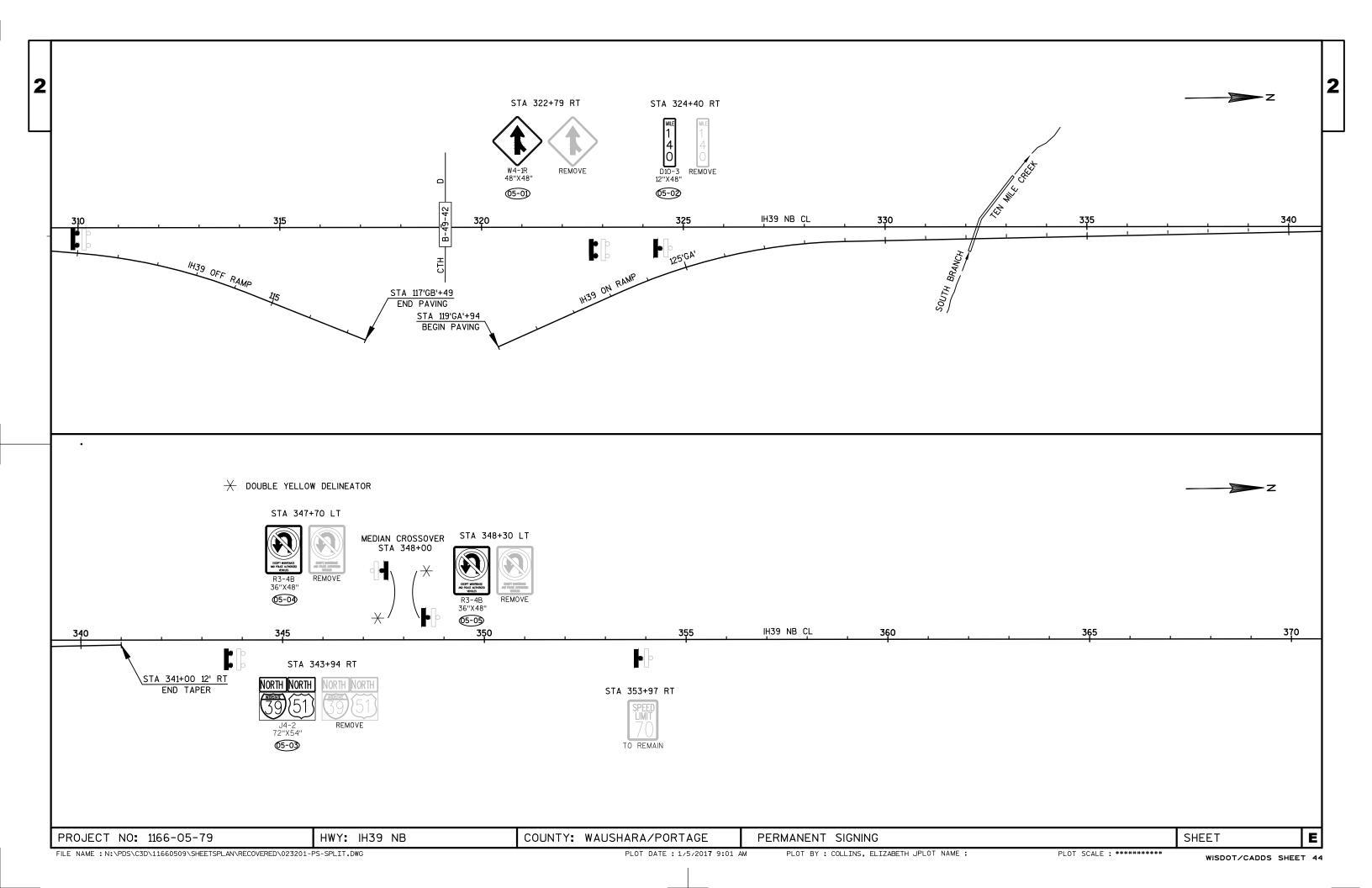


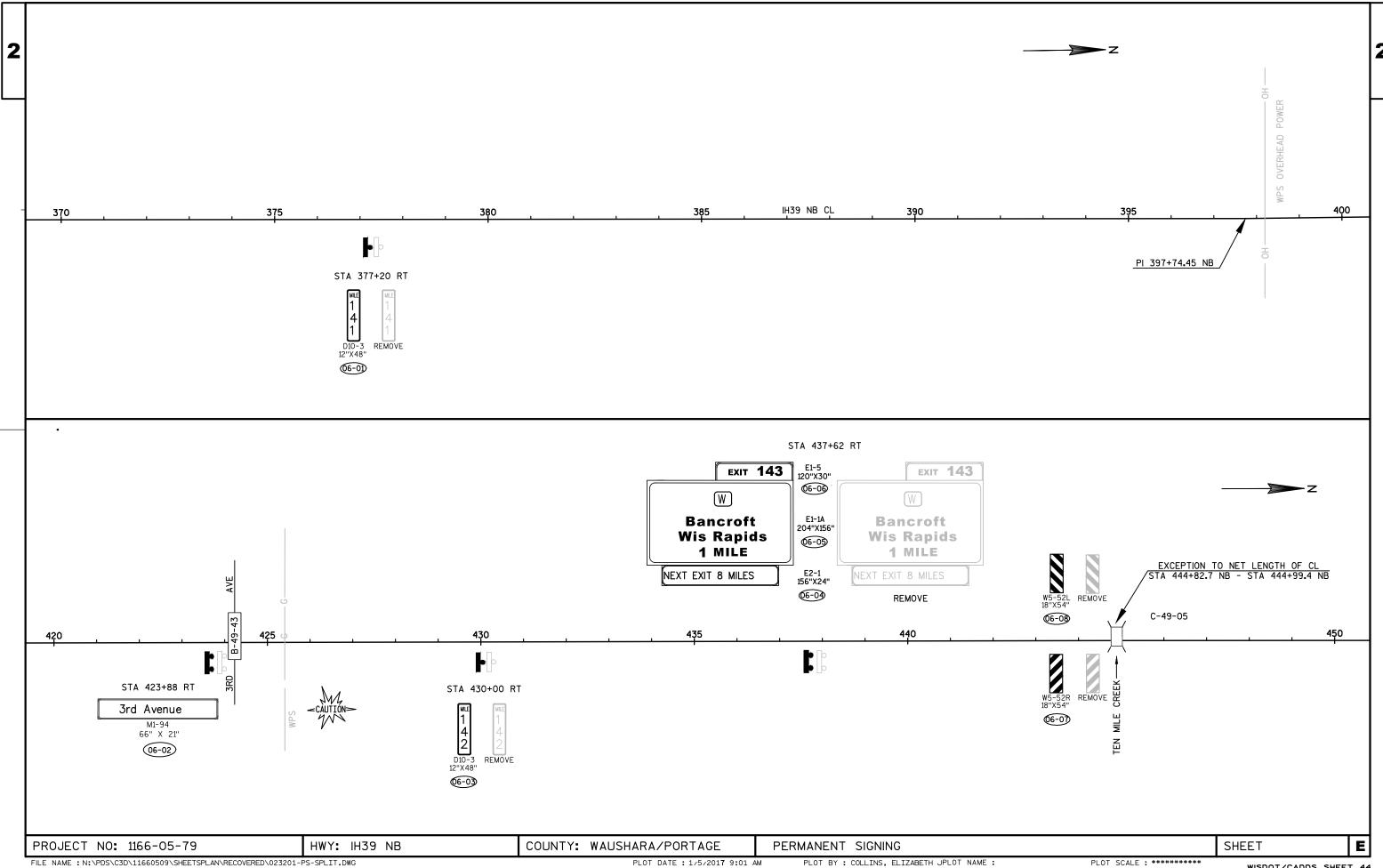


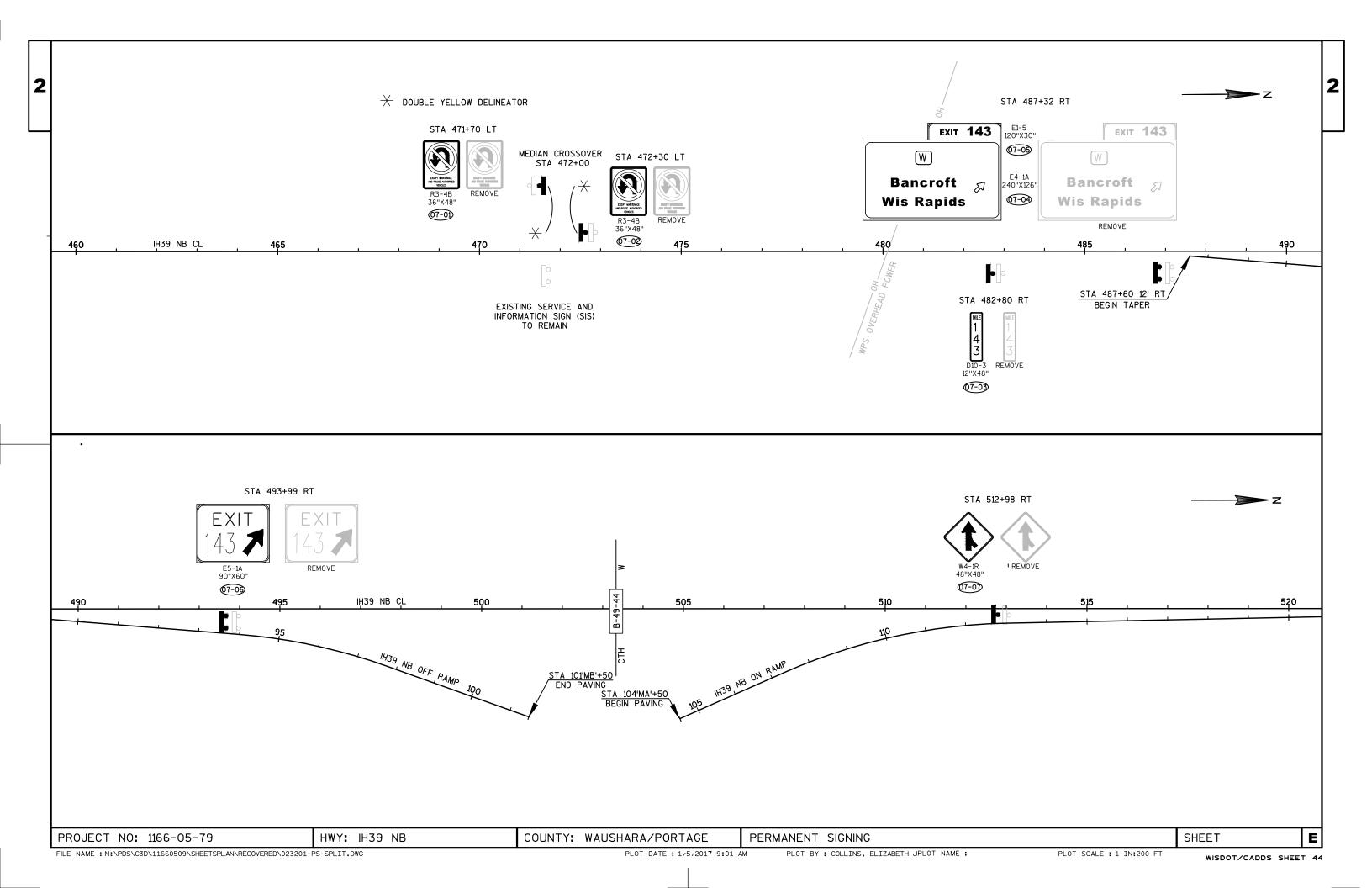


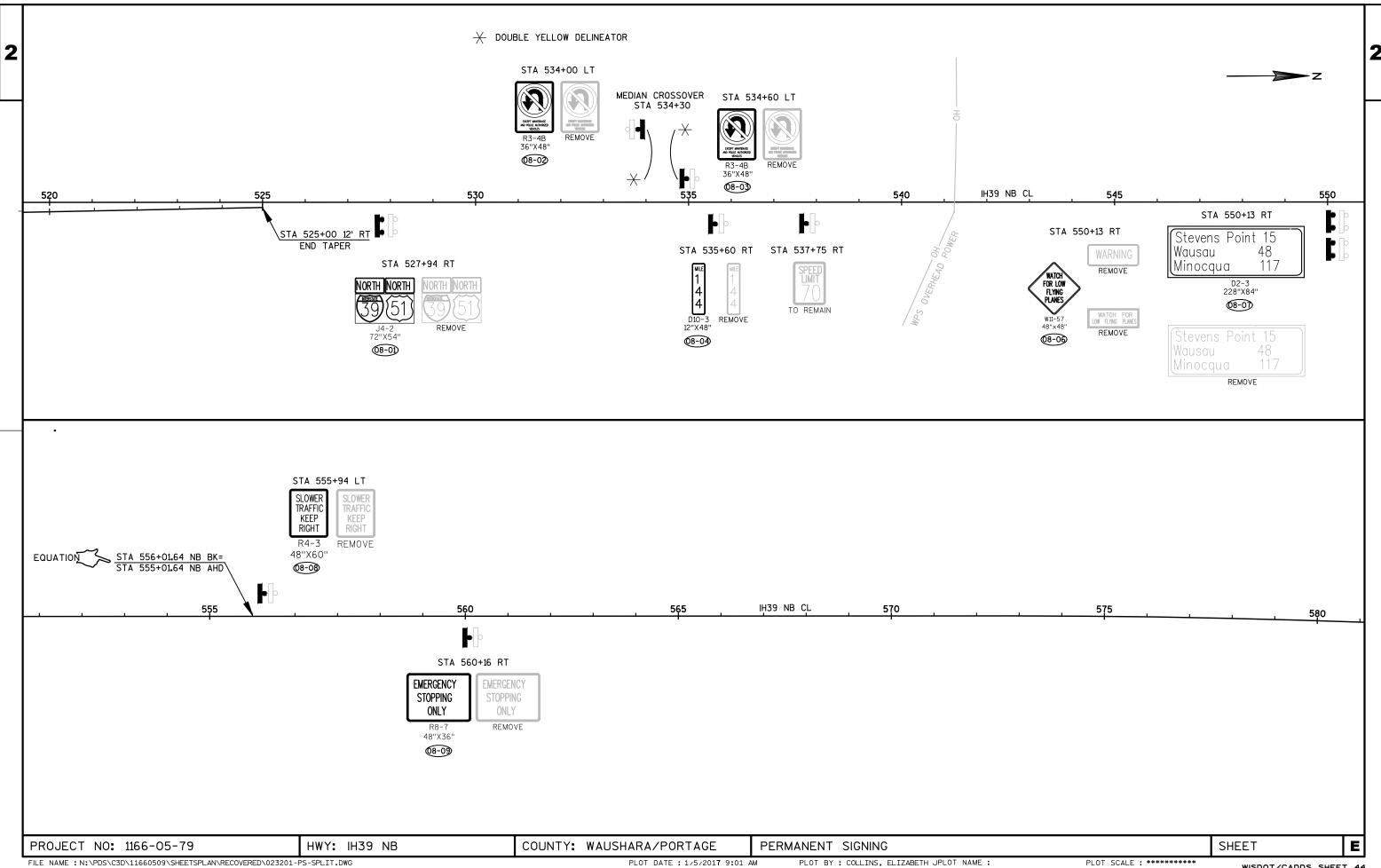


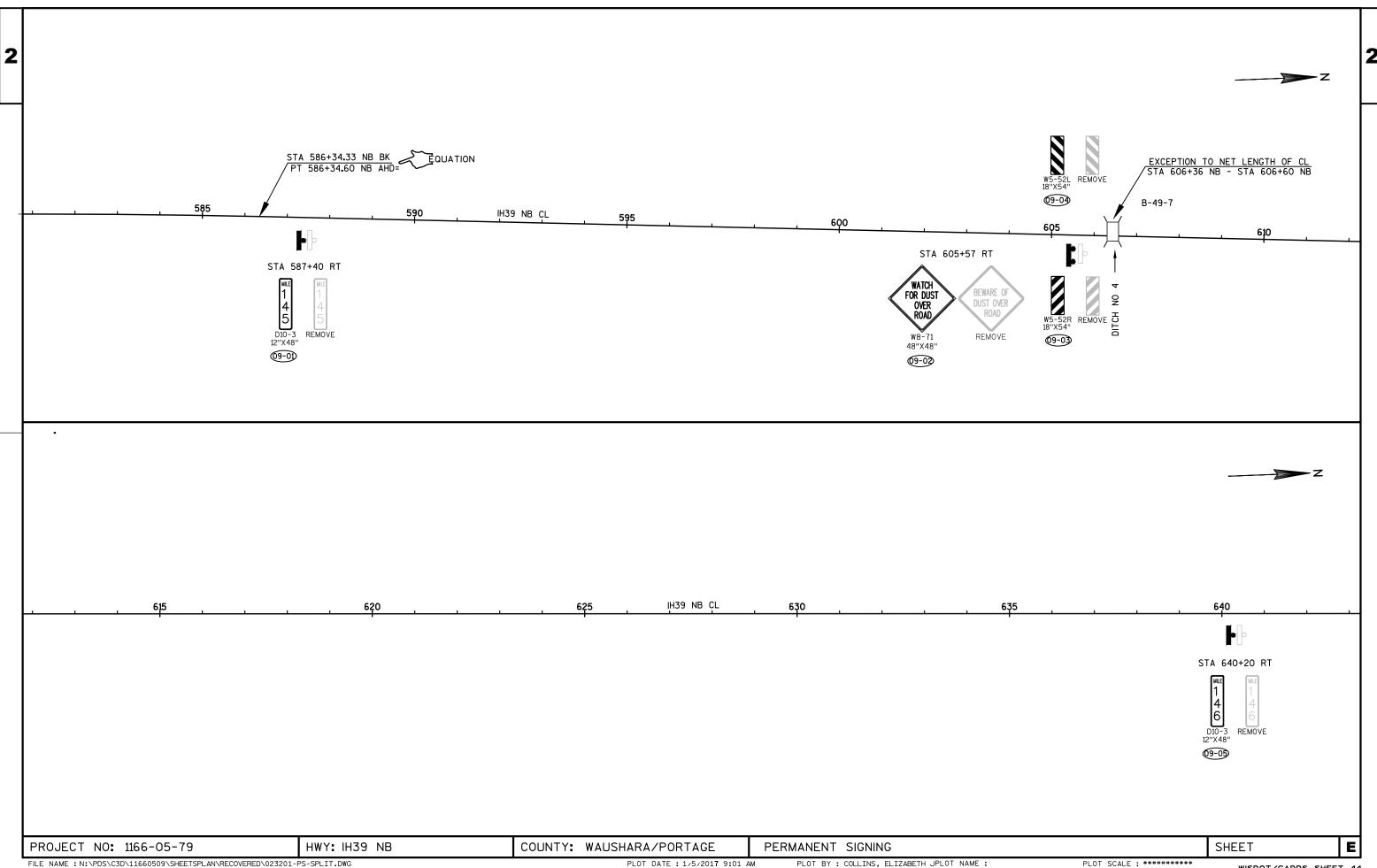


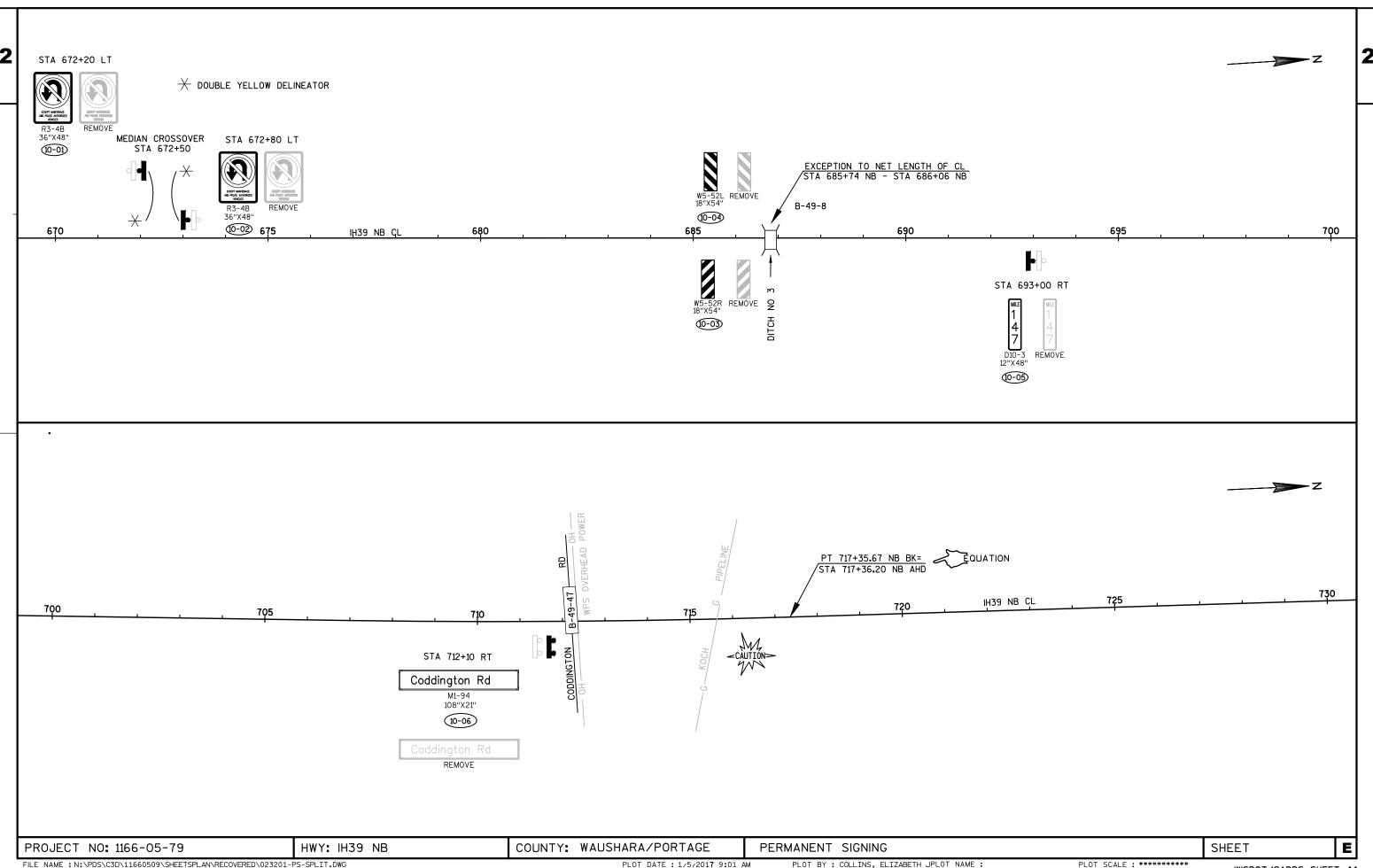


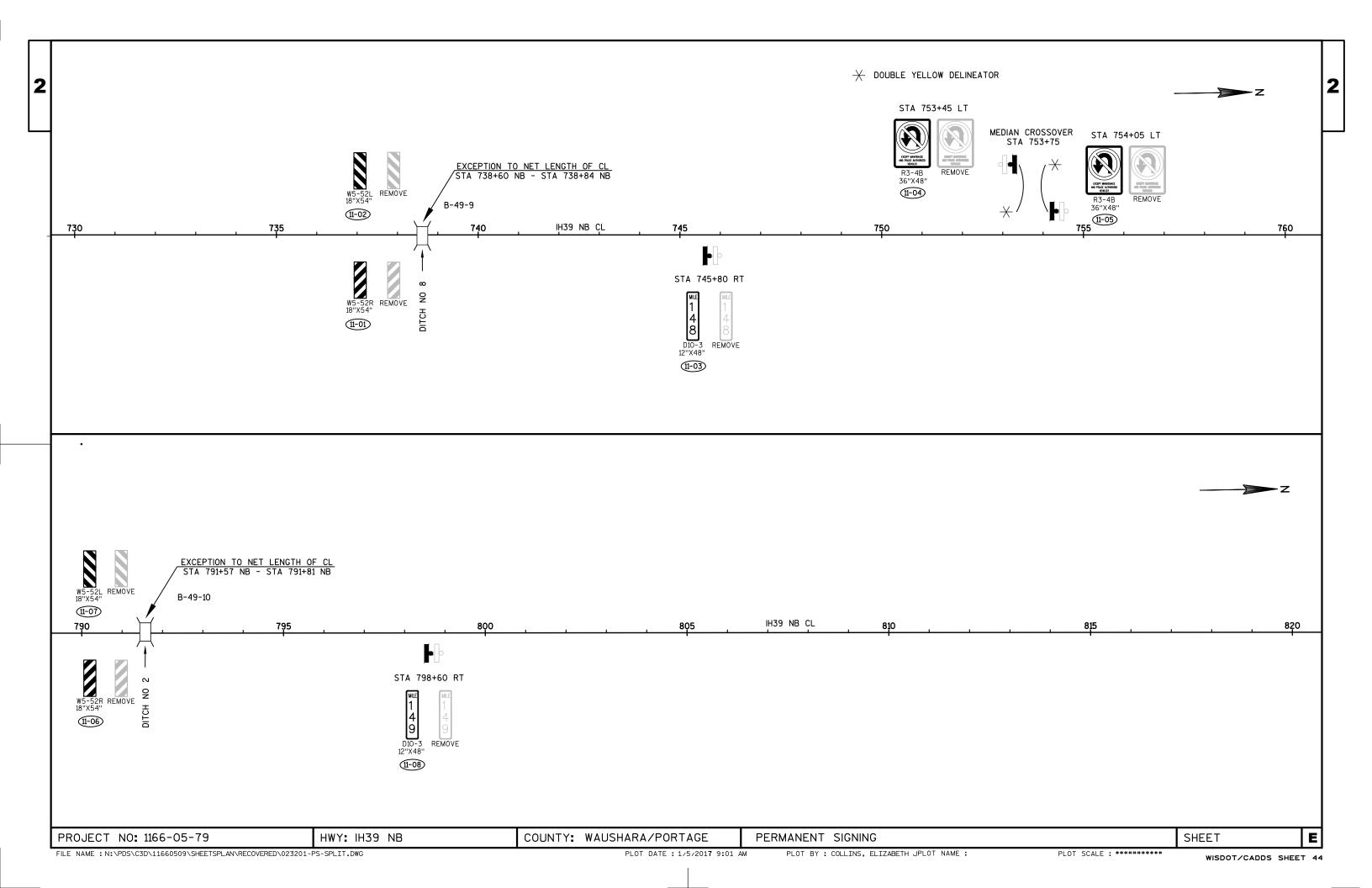


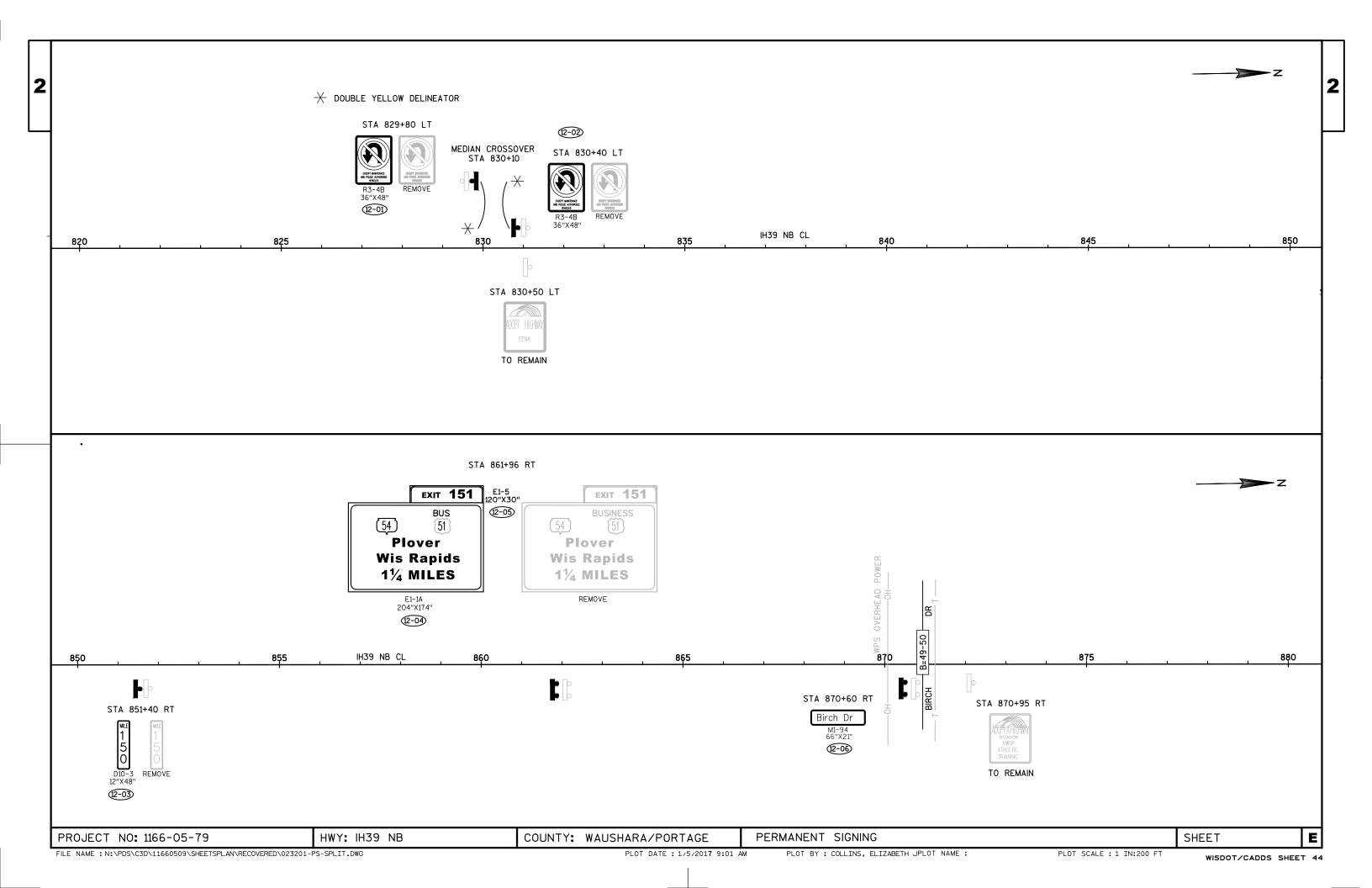


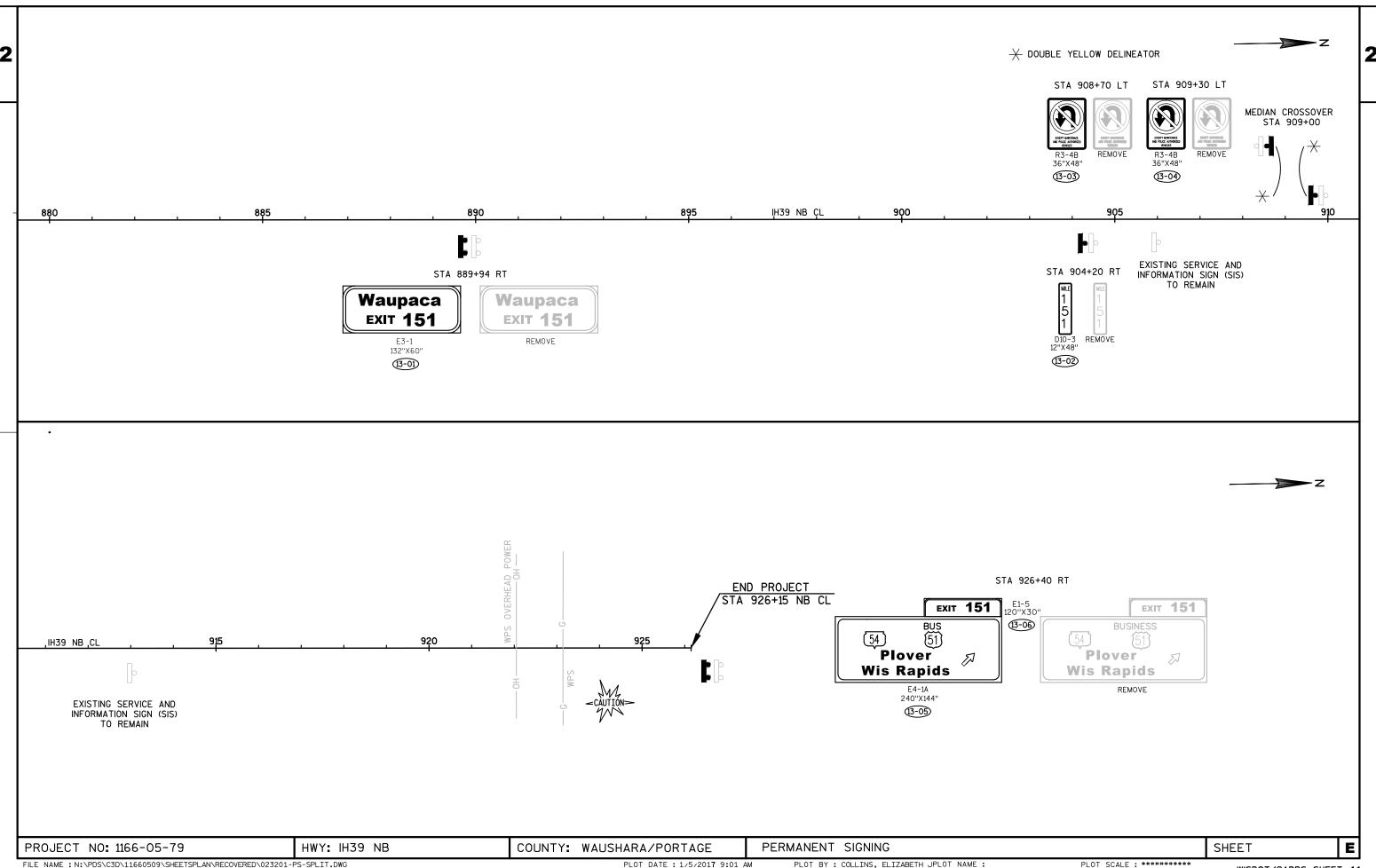


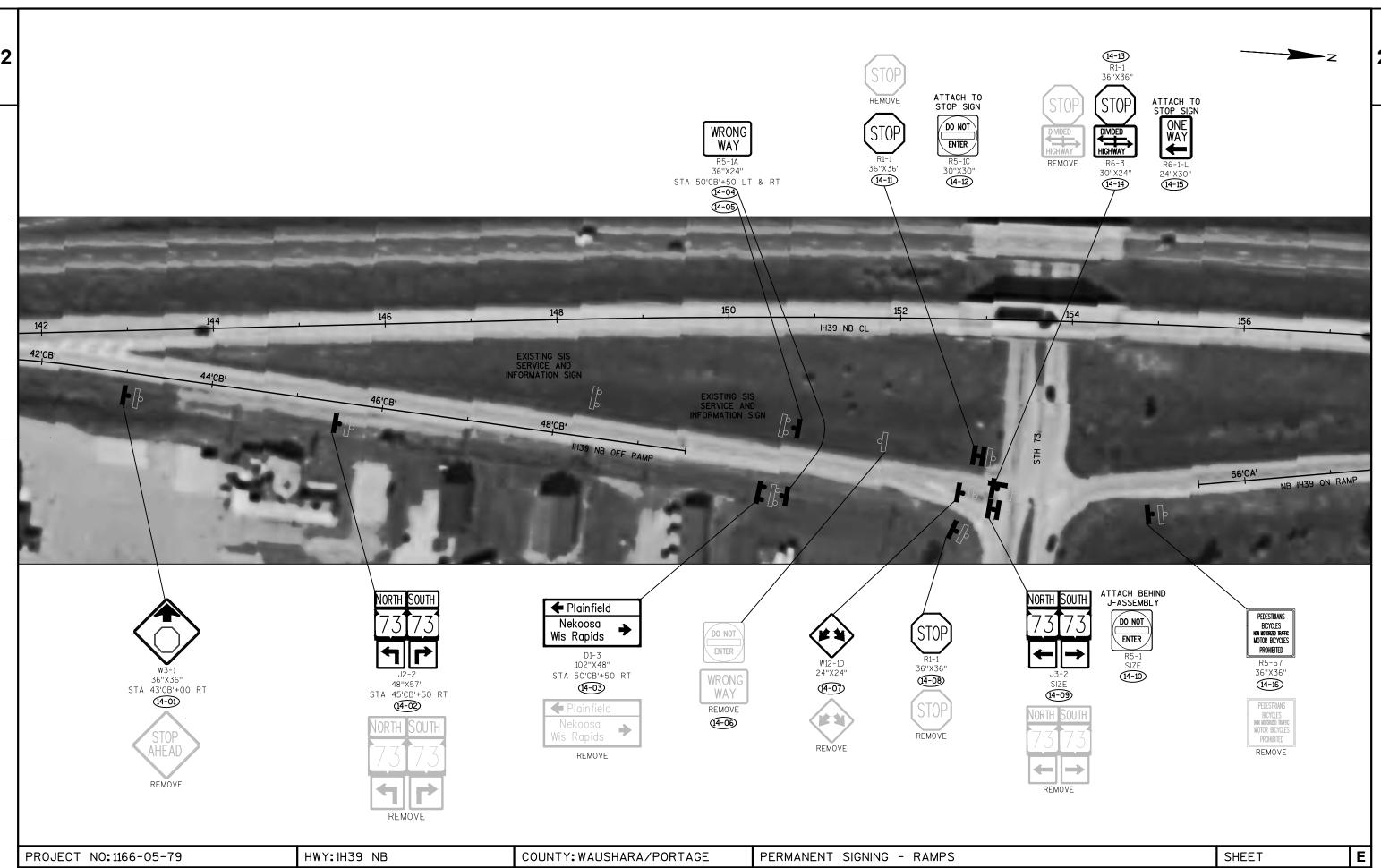


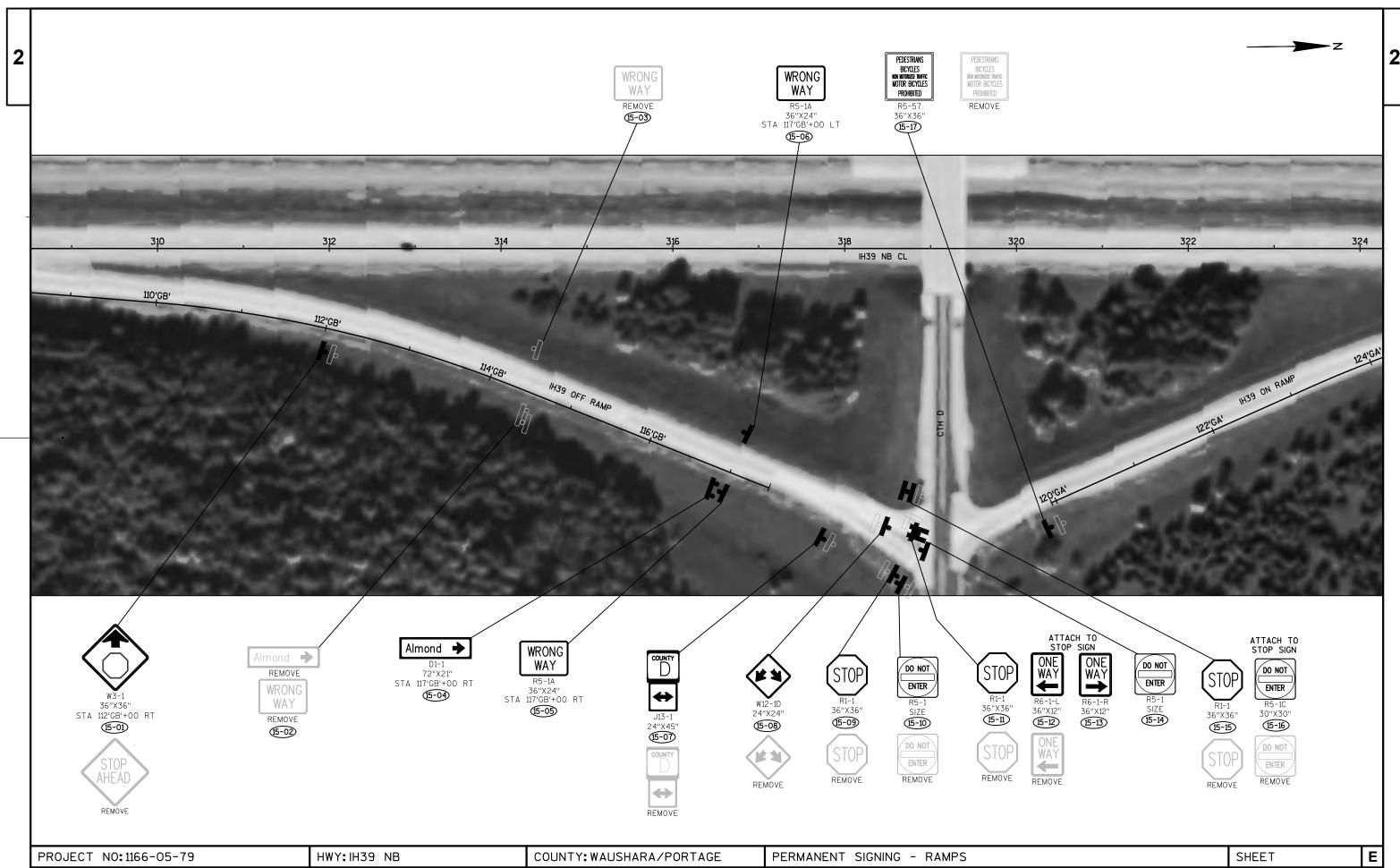


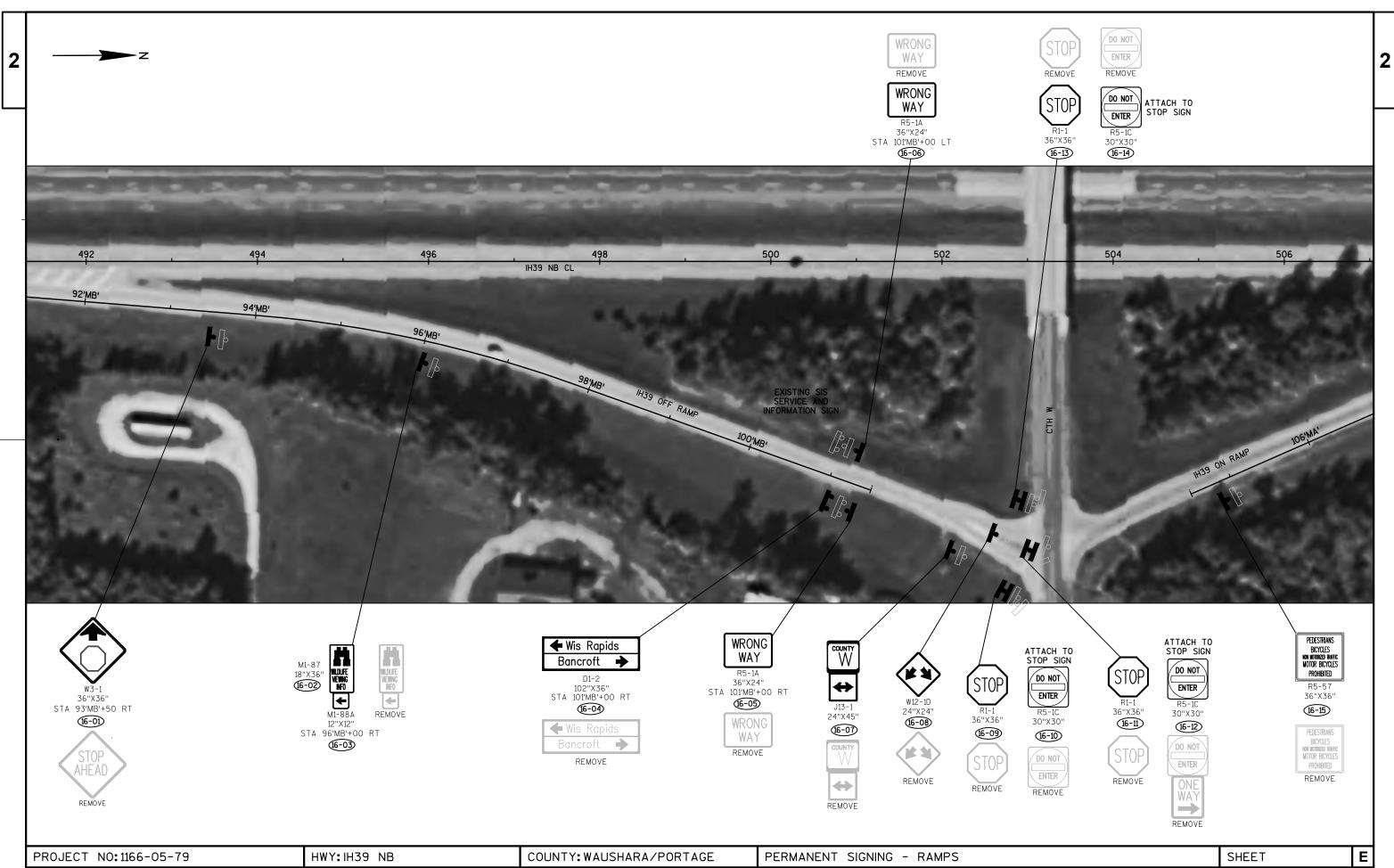












#### LEGEND

TRAFFIC CONTROL DRUM FLASHING ARROW BOARD

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

# CLOSED 1500 FT CĻOSED $\Rightarrow$ $\Rightarrow$ 50' SPACING LANE CLOSED <del>---350'---</del> 575' 2600' 1600' 1000' 780' MIN 500' MIN 780' MIN 300' MIN BUFFER SPACE TRANSITION AREA MILL OUT 1.75" OF THE 3.8' WIDE EXISTING RUMBLE STRIP

NOTES:

THIS LANE CLOSURE DETAIL APPLIES TO THE RIGHT LANE. FOR FILLING THE EXISTING SHOULDER RUMBLE STRIP WITH ASPHALT IN THE TRANSITION AREA FOR STAGE 2.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED. "W" SIGNS HAVE AN ORANGE BACKGROUND.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER

PROJECT NO: 1166-05-79

COUNTY: WAUSHARA/PORTAGE

TRAFFIC CONTROL STAGE 1

AREA AND PAVE 1.75" ASPHALTIC SURFACE IN THE TRANSITION

SHEET

HWY: IH39

#### LEGEND

- TRAFFIC CONTROL DRUM
- ▼ TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- F SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC

PCMS PORTABLE CHANGEABLE MESSAGE SIGN

- EXISTING SIGN POST
- SIGN ON PERMANENT SUPPORT TO BE LEFT IN PLACE FOR THE DURATION OF THE PROJECT

\* SPEED REDUCTION

#### NOTES:

THIS LANE CLOSURE DETAIL APPLIES TO THE PASSING LANE, REVERSE FOR THE DRIVING LANE.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED
TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE

A MINIMUM OF 200 FEET (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. "W" SIGNS HAVE AN ORANGE BACKGROUND.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL

BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

#### NOTES:

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION,
THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

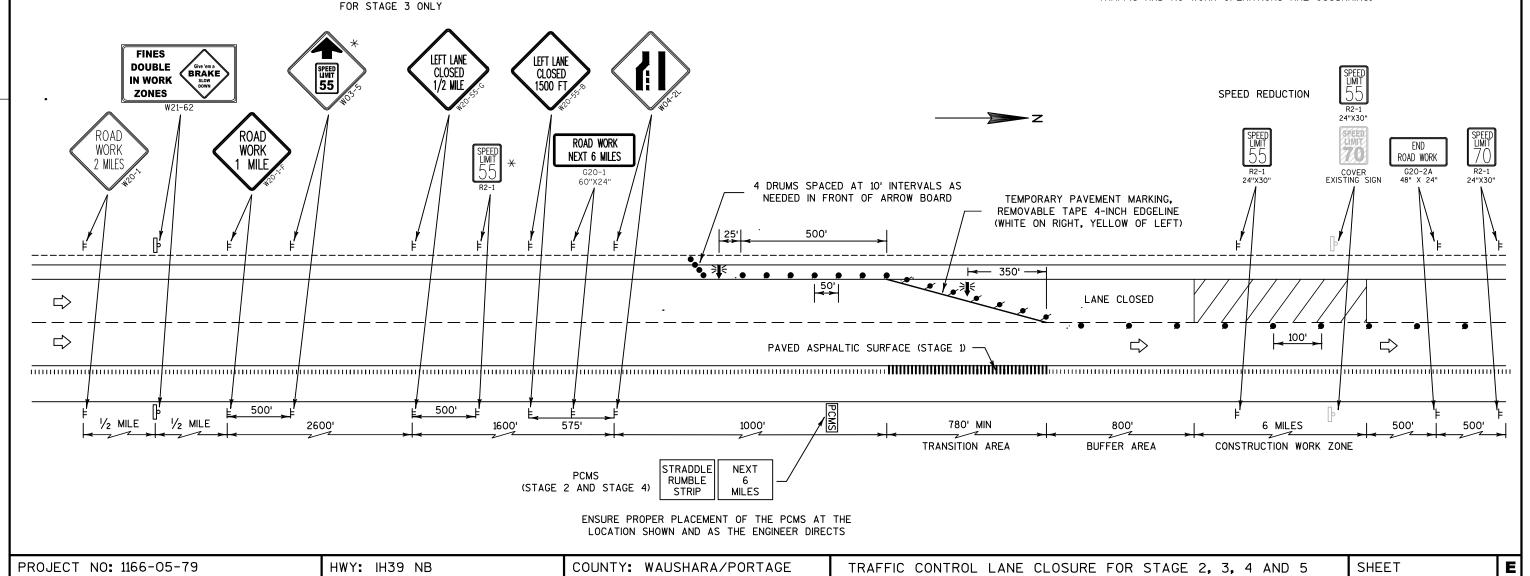
ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS
NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE
MUST TAKE PLACE FAR
ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW
FOR ADEQUATE BUFFER
SPACE.

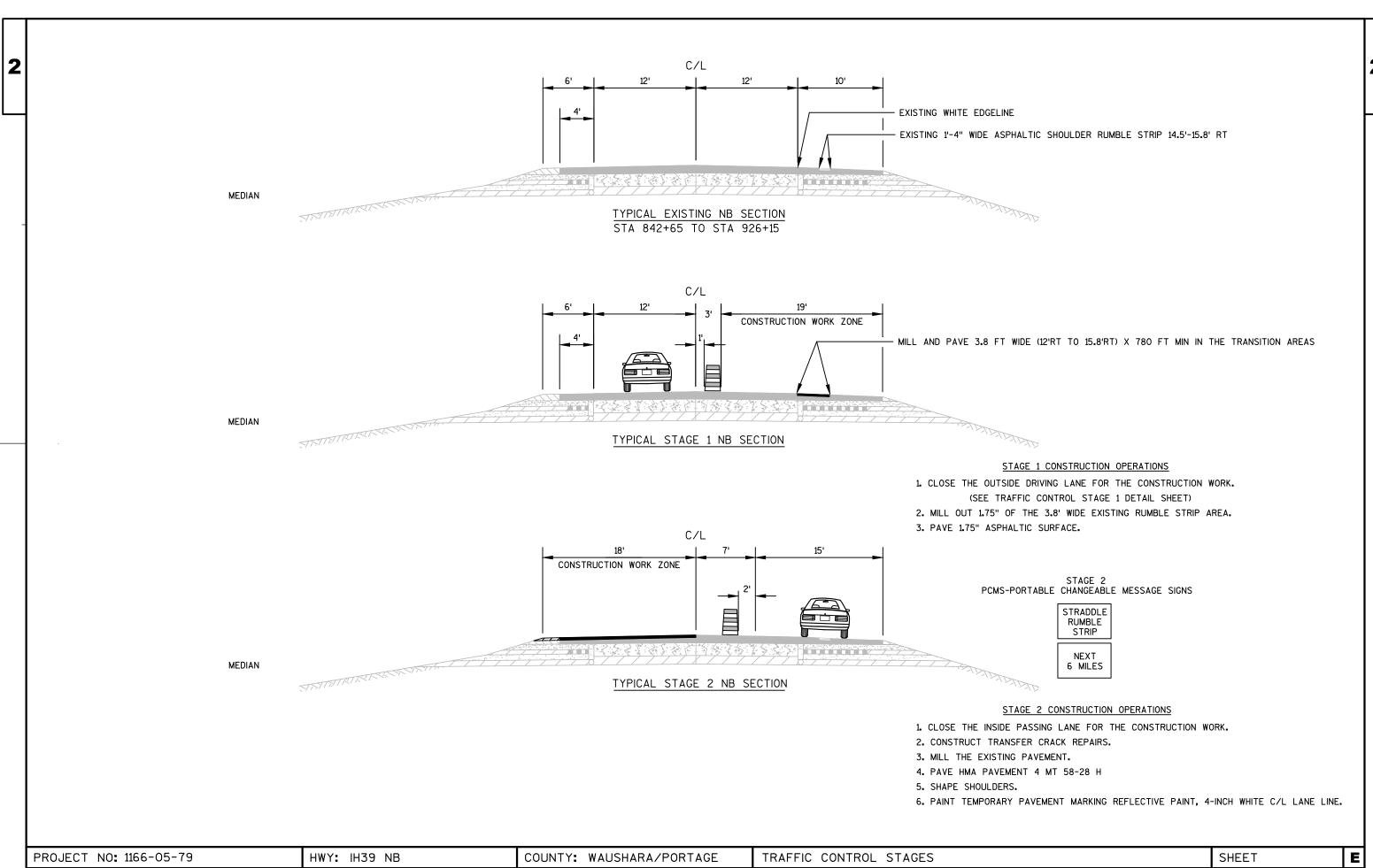
CONTRACTOR WORK AREA CONSISTS OF A STRETCH OF ROADWAY THAT CAN BE COMPLETED AND OPENED TO TRAFFIC NO LATER THAN 6:00PM THURSDAY OF EACH WORK WEEK.

THE TEMPORARY COVERING OF AN EXISTING SIGN IS NOT PAID FOR SEPARATELY, BUT IS INCLUDED IN OTHER ITEMS OF WORK.

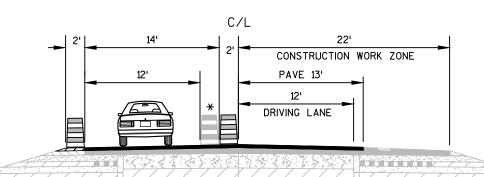
A 55 MPH SPEED LIMIT SIGN SHALL BE INSTALLED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF AN ENTRANCE RAMP. INSTALL 55 MPH SPEED LIMIT SIGNS EVERY 2 MILES IN THE CONSTRUCTION WORK ZONE AREA. INSTALL A "RESUME" 70 MPH SPEED LIMIT SIGN 500 FEET BEYOND THE "END OF ROADWORK" SIGN.

RESTORE 70 MPH SPEED LIMIT WHEN LANE CLOSURES ARE NO LONGER IN PLACE, WHEN BOTH THE DRIVING AND PASSING LANES ARE OPEN TO TRAFFIC AND NO WORK OPERATIONS ARE OCCURRING.









MEDIAN

#### TYPICAL STAGE 3 NB SECTION

\* TEMPORARILY MOVE DRUMS TO ALLOW FOR THE TRANSFER CRACK REPAIR OPERATION, THE MILLING OPERATION AND THE PAVING OPERATION.

RETURN DRUMS TO THE CENTERLINE FOLLOWING EACH OPERATION

# C/L 18' CONSTRUCTION WORK ZONE 2' 2'

MEDIAN

TYPICAL STAGE 4 NB SECTION

#### STAGE 3 CONSTRUCTION OPERATIONS

- 1. CLOSE THE OUTSIDE DRIVING LANE FOR THE CONSTRUCTION WORK.
- 2. CONSTRUCT TRANSFER CRACK REPAIRS.
- 3. MILL THE EXISTING PAVEMENT.
- 4. INSTALL ASPHALTIC SHOULDER RUMBLE STRIPS IN THE TRANSITION AREAS OF STAGE 1.
- 5. PAVE HMA PAVEMENT 4 MT 58-28 H
- 6. PAINT PAVEMENT MARKING EPOXY, 4-INCH WHITE EDGE LINE, DRIVING LANE AND RAMPS.
- 7. PAINT PAVEMENT MARKING EPOXY, 4-INCH WHITE DOTTED EDGE EXTENSIONS AT EXIT RAMPS.

STAGE 4
PCMS-PORTABLE CHANGEABLE MESSAGE SIGNS

STRADDLE RUMBLE STRIP

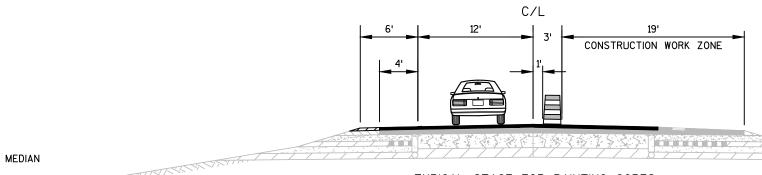
NEXT 6 MILES

#### STAGE 4 CONSTRUCTION OPERATIONS

- 1. CLOSE THE INSIDE PASSING LANE FOR THE CONSTRUCTION WORK.
- 2. INSTALL PASSING LANE SHOULDER RUMBLE STRIP.
- 3. PAINT PAVEMENT MARKING EPOXY, 4-INCH YELLOW EDGE LINE, PASSING LANE.

PROJECT NO: 1166-05-79 HWY: 1H39 NB COUNTY: WAUSHARA/PORTAGE TRAFFIC CONTROL STAGES SHEET





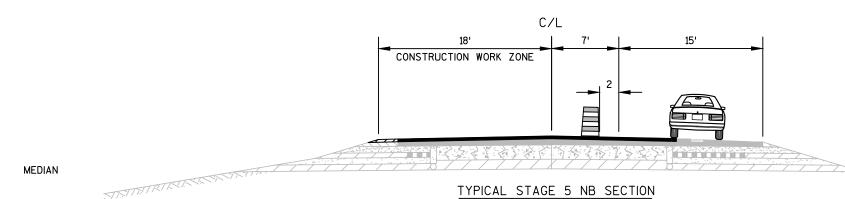
TYPICAL STAGE FOR PAINTING GORES

#### CONSTRUCTION OPERATIONS

- 1. CLOSE THE OUTSIDE DRIVING LANE AT THE GORE AREAS FOR THE CONSTRUCTION WORK.
- 2. INSTALL GROOVED WET REFLECTIVE TAPE, 8-INCH WHITE IN GORE.
- 3. PAINT PAVEMENT MARKING EPOXY, 24-INCH WHITE CHEVRONS IN GORE.

NOTE: GROOVED WET REFLECTIVE TAPE TO BE INSTALLED FIVE OR MORE DAYS AFTER PAVING.

DURING THE TIME THE GORES HAVE NO PAVEMENT MARKINGS, USE TRAFFIC CONTROL DRUMS.



STAGE 5
PCMS-PORTABLE CHANGEABLE MESSAGE SIGNS

STRADDLE RUMBLE STRIP

6 MILES

#### STAGE 5 CONSTRUCTION OPERATIONS

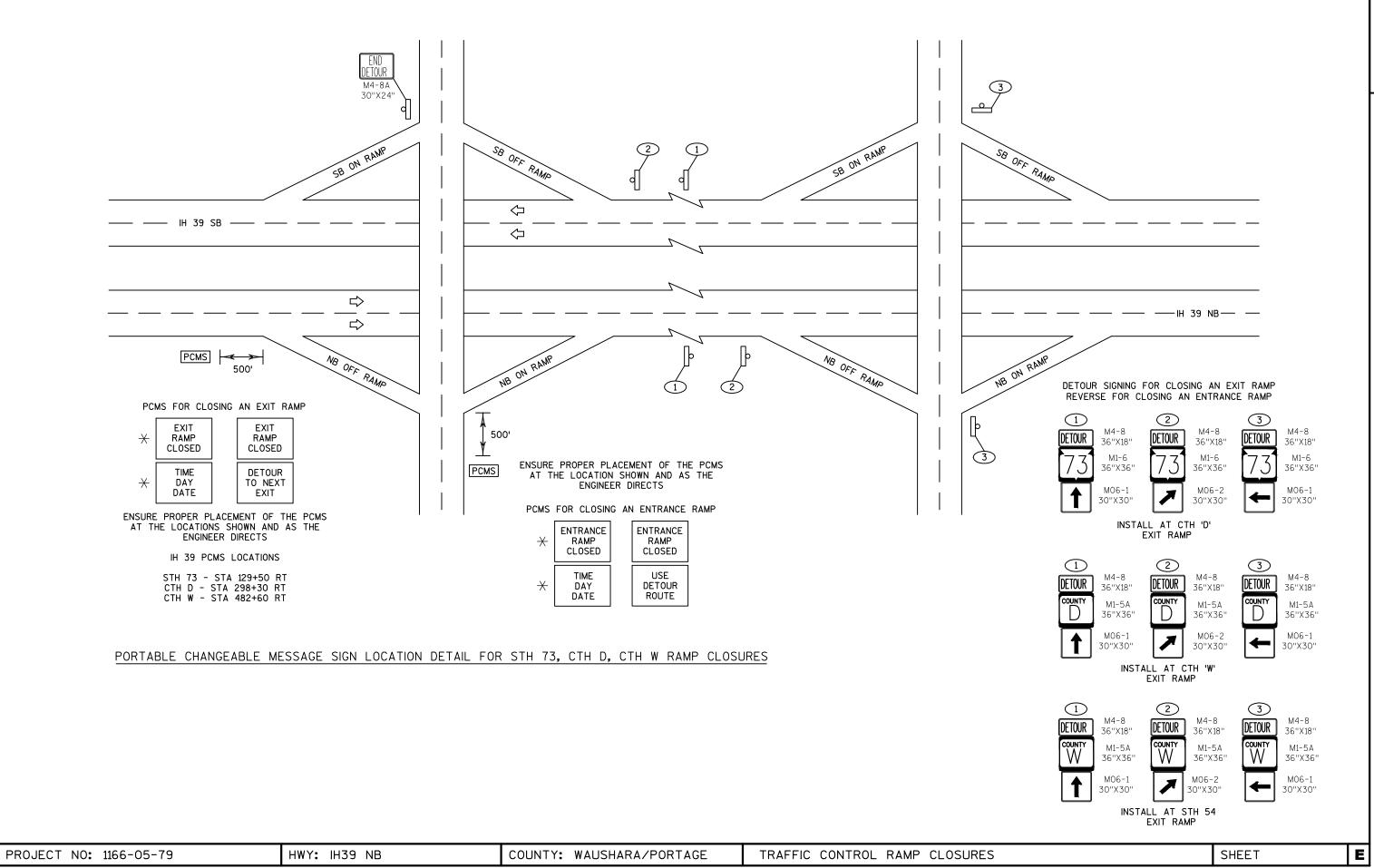
1. INSTALL GROOVED WET REFLECTIVE TAPE, 4-INCH WHITE C/L LANE LINE.

NOTE: GROOVED WET REFLECTIVE TAPE TO BE INSTALLED FIVE OR MORE DAYS AFTER PAVING.

STAGE 5 TO BE DONE WHEN ALL OF THE PROJECT PAVING IS COMPLETE.

PROJECT NO: 1166-05-79 HWY: 1H39 NB COUNTY: WAUSHARA/PORTAGE TRAFFIC CONTROL STAGES SHEET **E** 

FILE NAME: N:\PDS\C3D\11660509\SHEETSPLAN\025001-TC-TS.DWG PLOT DATE: 1/18/2017 9:20 AM PLOT BY: COLLINS, ELIZABETH JPLOT NAME: PLOT SCALE: 1 IN:10 FT WISDOT/CADDS SHEET 42



IH39 SB CL IH39 SB CL IH39 NB CL IH39 NB CL INSTALL 1000' FROM GORE COVER OR REMOVE WHEN NO LONGER RELEVANT COVER OR REMOVE WHEN NO LONGER RELEVANT W12-52 48"X48" XX MILES XX MILES AHEAD AHEAD W057-52 48"X36" COUNTY: WAUSHARA/PORTAGE PROJECT NO:1166-05-79 HWY: IH39 NB TRAFFIC CONTROL STAGE 3 WIDTH RESTRICTION SHEET

FILE NAME : N:\PDS\C3D\11660509\SHEETSPLAN\025001-TC-WIDELOAD.DWG 023201\_PS - 023216

2





FILE NAME: N:\PDS\C3D\11660509\SHEETSPLAN\025001-TC-WIDELOAD.DWG 023201\_PS - 023214

IH39 SB CL IH39 SB CL IH39 NB CL IH39 NB CL 128'GA' 108'GB' INSTALL 1000' FROM GORE COVER OR REMOVE WHEN NO LONGER RELEVANT COVER OR REMOVE WHEN NO LONGER RELEVANT **W**12-52 48"X48" **W12-52** 48"X48" XX MILES XX MILES AHEAD AHEAD W057-52 48"X36" W057-52 48"X36" COUNTY: WAUSHARA/PORTAGE PROJECT NO: 1166-05-79 HWY: IH39 NB TRAFFIC CONTROL STAGE 3 WIDTH RESTRICTION SHEET IH39 SB CL IH39 SB CL IH39 NB CL IH39 NB CL INSTALL 1000' FROM GORE COVER OR REMOVE WHEN NO LONGER RELEVANT COVER OR REMOVE WHEN NO LONGER RELEVANT **W**12-52 48"X48" W12-52 48"X48" XX MILES XX MILES AHEAD AHEAD W057-52 48"X36" W057-52 48"X36"

FILE NAME : N:\PDS\C3D\11660509\SHEETSPLAN\025001-TC-WIDELOAD.DWG 023201\_PS - 023216

HWY: IH39 NB

PROJECT NO: 1166-05-79

PLOT DATE: 10/24/2016 1:25 PM

COUNTY: WAUSHARA/PORTAGE

PLOT BY : COLLINS, ELIZABETH JPLOT NAME :

TRAFFIC CONTROL STAGE 3 WIDTH RESTRICTION

PLOT SCALE : 1 IN:200 FT

Ε

SHEET

					1166-05-79	
Line	Item	Item Description	Unit	Total	Qty	
0390	643.3000	Traffic Control Detour Signs	DAY	42.000	42.000	
0400	646.0106	Pavement Marking Epoxy 4-Inch	LF	189,250.000	189,250.000	
0410	646.0881.S	Pavement Marking Grooved Wet Reflective Tape 4-Inch	LF	22,331.000	22,331.000	
0420	646.0883.S	Pavement Marking Grooved Wet Reflective Tape 8-Inch	LF	5,304.000	5,304.000	
0430	647.0746	Pavement Marking Diagonal Epoxy 24-Inch	LF	442.000	442.000	
0440	649.0400	Temporary Pavement Marking Removable Tape 4-Inch	LF	18,720.000	18,720.000	
0450	649.0402	Temporary Pavement Marking Paint 4-Inch	LF	7,147.000	7,147.000	
0460	650.8000	Construction Staking Resurfacing Reference	LF	89,448.000	89,448.000	
0470	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	120.000	120.000	
0480	653.0135	Pull Boxes Steel 24x36-Inch	EACH	6.000	6.000	
0490	654.0105	Concrete Bases Type 5	EACH	6.000	6.000	
0500	654.0220	Concrete Control Cabinet Bases Type 10	EACH	6.000	6.000	
0510	657.0100	Pedestal Bases	EACH	6.000	6.000	
0520	657.0255	Transformer Bases Breakaway 11 1/2-Inch Bolt Circle	EACH	6.000	6.000	
0530	657.0425	Traffic Signal Standards Aluminum 15-FT	EACH	6.000	6.000	
0540	662.2037.S	Ramp Closure Gates Solar 37-FT	EACH	1.000	1.000	
0550	662.2040.S	Ramp Closure Gates Solar 40-FT	EACH	5.000	5.000	
0560	662.3037.S	Ramp Closure Gate Arms Stockpile 37-FT	EACH	2.000	2.000	
0570		•	EACH	2.000	2.000	
0580	662.4000.S	Ramp Closure Gate Flashers Stockpile	EACH	2.000	2.000	
0590	690.0150	Sawing Asphalt	LF	168.000	168.000	
0600	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	2,000.000	2,000.000	
0610	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	1,260.000	1,260.000	
0620	SPV.0060	Special 01. HMA Percent Within Limits (PWL) Test Strip		1.000	1.000	
0630	SPV.0090	Special 01. Sealing Asphaltic Pavement Cracks	LF	72,550.000	72,550.000	

# REMOVING ASPHALTIC SURFACE

		204.0110	
STATION - STATION	LOCATION	SY	REMARKS
STA 40'CB'+81 - STA 40'CB'+85	'CB' RAMP	5	TRANSVERSE CRACK REPAIR
STA 40'CB'+88 - STA 41'CB'+10	'CB' RAMP	29	TRANSVERSE CRACK REPAIR
STA 140+96 - STA 141+02	DRIVING LANE	8	TRANSVERSE CRACK REPAIR
STA 141+04 - STA 141+08	PASSING LANE	5	TRANSVERSE CRACK REPAIR
UNDISTRIBUTED		40	TRANSVERSE CRACK REPAIR
	TOTAL	87	

# **SHAPING SHOULDERS**

305.0500

STATION - STATION	LOCATION	STATION	REMARKS
STA 842+65 - STA 926+15	LT	893	2' PASSING LANE SHOULDER
STA 37'CB'+09 - STA 49'CB'+56	RT	12	'CB' RAMP (STH 73) 3' SHOULDER
STA 43'CB'+05 - STA 49'CB'+56	LT	7	'CB' RAMP (STH 73) 3' SHOULDER
STA 55'CA'+45.8 - STA 75'CA'+00	RT	20	'CA' RAMP (STH 73) 3' SHOULDER
STA 55'CA'+45.8 - STA 64'CA'+50	LT	9	'CA' RAMP (STH 73) 5' SHOULDER
STA 103'GB'+51 - STA 117'GB'+49	RT	14	'GB' RAMP (CTH D) 3' SHOULDER
STA 108'GB'+59 - STA 117'GB'+49	LT	9	'GB' RAMP (CTH D) 5' SHOULDER
STA 119'GA'+94 - STA 137'GA'+42	RT	17	'GA' RAMP (CTH D) 3' SHOULDER
STA 119'GA'+94 - STA 129'GA'+82	LT	10	'GA' RAMP (CTH D) 5' SHOULDER
STA 87'MB'+60 - STA 101'MB'+50	RT	14	'MB' RAMP (CTH W) 3' SHOULDER
STA 92'MB'+49 - STA 101'MB'+50	LT	9	'MB' RAMP (CTH W) 5' SHOULDER
STA 104'MA'+50 - STA 122'MA'+46	RT	18	'MA' RAMP (CTH W) 3' SHOULDER
STA 104'MA'+50 - STA 115'MA'+00	LT	11	'MA' RAMP (CTH W) 5' SHOULDER
	TOTAL	1042	

# REMOVING ASPHALTIC SURFACE MILLING

		204.0125	
STATION - STATION	LOCATION	TON	REMARKS/WIDTH
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	RT	1144	13'
STA 94+99.6 AHD - STA 144+00 CONC PAVT	RT	681	13'
STA 163+00 CONC PAVT - STA 232+89.45 BK	RT	972	13'
STA 233+89.45 AHD - STA 444+82.7 C-49-5	RT	2933	13'
STA 444+99.4 C-49-5 - STA 556+01.64 BK	RT	1543	13'
STA 555+01.64 AHD - STA 606+36 B-49-7	RT	714	13'
STA 606+60 B-49-7 - STA 685+74 B-49-8	RT	1100	13'
STA 686+06 B-49-8 - STA 738+60 B-49-9	RT	730	13'
STA 738+84 B-49-9 - STA 791+57 B-49-10	RT	733	13'
STA 791+81 B-49-10 - STA 926+15 END PROJECT	RT	1868	13'
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	LT	1409	16'
STA 94+99.6 AHD - STA 144+00 CONC PAVT	LT	838	16'
STA 163+00 CONC PAVT - STA 232+89.45 BK	LT	1196	16'
STA 233+89.45 AHD - STA 444+82.7 C-49-5	LT	3609	16'
STA 444+99.4 C-49-5 - STA 556+01.64 BK	LT	1900	16'
STA 555+01.64 AHD - STA 606+36 B-49-7	LT	878	16'
STA 606+60 B-49-7 - STA 685+74 B-49-8	LT	1354	16'
STA 686+06 B-49-8 - STA 738+60 B-49-9	LT	898	16'
STA 738+84 B-49-9 - STA 791+57 B-49-10	LT	902	16'
STA 791+81 B-49-10 - STA 926+15 END PROJECT	LT	2299	16'
STA 137+09 RT - STA 49'CB'+56	'CB' RAMP	275	INCLUDES GORE AREA
STA 55'CB'+46 - STA 175+00 RT	'CA' RAMP	417	INCLUDES GORE AREA
STA 303+51 RT - STA 117'GB'+49	'GB' RAMP	342	INCLUDES GORE AREA
STA 119'GA'+94 - STA 337+42 RT	'GA' RAMP	409	INCLUDES GORE AREA
STA 487+60 RT - STA 101'MB'+50	'MB' RAMP	355	INCLUDES GORE AREA
STA 104'MA'+50 - STA 522+46 RT	'MA' RAMP	394	INCLUDES GORE AREA
RUMBLE STRIP AREAS	RT	190	TRANSITION AREAS STAGE 1
	TOTAL	30083	

# BASE AGGREGATE DENSE

LOCATION

STATION - STATION

305.0110

3/4-INCH TON

STA 842+65 - STA 926+15 SHOULDERS 800 SHAPING SHOULDERS

LINDISTRIBLITED

UNDISTRIBUTED

REMARKS

TOTAL 800

PROJECT NO: 1166-05-79 HWY: IH39 NB COUNTY: WAUSHARA/PORTAGE MISCELLANEOUS QUANTITIES SHEET: **E** 

# **HMA PAVEMENT**

STATION - STATION	LOCATION	455.0605 TACK COAT GAL	460.6424 HMA PAVEMENT 4 MT 58-28 H TON
- Charlest Charlest	200/111011	0,12	
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	RT	297	1144
STA 94+99.6 AHD - STA 144+00 CONC PAVT	RT	177	681
STA 163+00 CONC PAVT - STA 232+89.45 BK	RT	252	972
STA 233+89.45 AHD - STA 444+82.7 C-49-5	RT	762	2933
STA 444+99.4 C-49-5 - STA 556+01.64 BK	RT	401	1543
STA 555+01.64 AHD - STA 606+36 B-49-7	RT	185	714
STA 606+60 B-49-7 - STA 685+74 B-49-8	RT	286	1100
STA 686+06 B-49-8 - STA 738+60 B-49-9	RT	190	730
STA 738+84 B-49-9 - STA 791+57 B-49-10	RT	190	733
STA 791+81 B-49-10 - STA 926+15 END PROJECT	RT	485	1868
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	LT	366	1409
STA 94+99.6 AHD - STA 144+00 CONC PAVT	LT	218	838
STA 163+00 CONC PAVT - STA 232+89.45 BK	LT	311	1196
STA 233+89.45 AHD - STA 444+82.7 C-49-5	LT	937	3609
STA 444+99.4 C-49-5 - STA 556+01.64 BK	LT	494	1900
STA 555+01.64 AHD - STA 606+36 B-49-7	LT	228	878
STA 606+60 B-49-7 - STA 685+74 B-49-8	LT	352	1354
STA 686+06 B-49-8 - STA 738+60 B-49-9	LT	233	898
STA 738+84 B-49-9 - STA 791+57 B-49-10	LT	234	902
STA 791+81 B-49-10 - STA 926+15 END PROJECT	LT	597	2299
STA 137+09 RT - STA 49'CB'+56	'CB' RAMP	71	275
STA 55'CB'+46 - STA 175+00 RT	'CA' RAMP	108	417
STA 303+51 RT - STA 117'GB'+49	'GB' RAMP	89	342
STA 119'GA'+94 - STA 337+42 RT	'GA' RAMP	106	409
STA 487+60 RT - STA 101'MB'+50	'MB' RAMP	92	355
STA 104'MA'+50 - STA 522+46 RT	'MA' RAMP	102	394
STA 140+96 - STA 141+08 AND 'CB' RAMP	TRANSVERSE CRACK REPAIRS	4	14
UNDISTRIBUTED	TRANSVERSE CRACK REPAIRS	3	12
	TOTALS	22467	29919

#### REHEATING HMA PAVEMENT LONGITUDINAL JOINTS

460.4110.S LOCATION LF REMARKS STA 842+65 - STA 926+15 89325 PROJECT LENGTH 89325

TOTAL

## **ASPHALTIC SURFACE**

	465.0105	
LOCATION	TON	REMARKS
RUMBLE STRIP AREAS	190	TRANSITION AREAS STAGE 1
TOTAL	190	

## ASPHALTIC SHOULDER RUMBLE STRIP

			465.0400
	STATION - STATION	LOCATION	LF
,			
	STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	LT	8233
	STA 94+99.60 AHD - STA 144+00 CONC PAVT	LT	4900
	STA 163+00 CONC PAVT - STA 232+89.45 BK	LT	6988
,	STA 233+89.45 AHD - STA 444+82.7 C-49-5	LT	21093
	STA 444+99.4 C-49-5 - STA 556+01.64 BK	LT	11102
	STA 555+01.64 AHD - STA 606+36 B-49-7	LT	5134
	STA 606+60 B-49-7 - STA 685+74 B-49-8	LT	7914
	STA 686+06 B-49-8 - STA 738+60 B-49-9	LT	5254
	STA 738+84 B-49-9 - STA 791-57 B-49-10	LT	5273
	STA 791+81 B-49-10 - STA 926+15 END PROJECT	LT	13434
	RUMBLE STRIPS IN TRANSITION AREAS	RT	4680
		TOTAL	94005

#### **DELINEATOR POSTS AND REFLECTORS**

		SPACING	204.0180 REMOVING DELINEATORS AND MARKERS	633.0100 DELINEATOR POSTS STEEL	633.0500 DELINEATOR REFLECTORS	
STATION-STATION	LOCATION	LF	EACH	EACH	EACH	REMARKS
						_
STA 842+65 - STA 137+10	RT	400	39	31	31	MAINLINE
STA 37'CB'+10 - STA 43'CB'+10	RT	100	6	7	14	RAMP TAPER DOUBLE DELINEATOR
STA 43'CB'+10 - STA 49'CB'+56	RT	100	6	6	6	'CB' EXIT RAMP
STA 143+10 - STA 164+05	RT					MAINLINE EXCEPTION
STA 55'CA'+46 - STA 64'CA'+05	RT	100	9	8	8	'CA' ENTRANCE RAMP
STA 64'CA'+05 - STA 74'CA'+98	RT	100	11	12	24	RAMP TAPER DOUBLE DELINEATOR
STA 174+98 - STA 303+54	RT	400	32	32	32	MAINLINE
STA 103'GB'+54 - STA 108'GB'+65	RT	100	6	6	12	RAMP TAPER DOUBLE DELINEATOR
STA 108'GB'+65 - STA 117'GB'+49	RT	100	8	9	9	'GB' EXIT RAMP
STA 308+65 - STA 327+00	RT	200	4	9	9	MAINLINE
STA 119'GA'+94 - STA 127'GA'+00	RT	100	6	7	7	'GA' ENTRANCE RAMP
STA 127'GA'+00 - STA 138'GA'+00	RT	100	10	12	24	RAMP TAPER DOUBLE DELINEATOR
STA 338+00 - STA 487+52	RT	400	36	37	37	MAINLINE
STA 87'MB'+52 - STA 92'MB'+90	RT	100	6	6	12	RAMP TAPER DOUBLE DELINEATOR
STA 92'MB'+90 - STA 101'MB'+50	RT	100	8	9	9	'MB' EXIT RAMP
STA 492+90 - STA 510+95	RT	200	3	9	9	MAINLINE
STA 104'MA'+50 - STA 110'MA'+95	RT	100	5	6	6	'MA' ENTRANCE RAMP
STA 110'MA'+95 - STA 122'MA'+49	RT	100	11	12	24	RAMP TAPER DOUBLE DELINEATOR
STA 522+49 - STA 926+15	RT	400	100	101	101	MAINLINE
STA 842+65 - STA 926+15	LT		20	20	40	MEDIAN CROSSOVERS
			326	339	414	

#### **SIGNS TYPE II**

						POSTS 4" )			637.2210 SIGNS	637.2230 SIGNS	638.2602 REMOVING	638.3000 REMOVING	
					634.0612	634.0614		634.0618	TYPE II	TYPE II	SIGNS	SMALL SIGN	
SIGN	SIGN				12-FT	14-FT	16-FT	18-FT		REFLECTIVE F	TYPE II	SUPPORTS	
NO.	CODE	STATION	MESSAGE	SIZE	EACH	EACH	EACH	EACH	SF	SF	EACH	EACH	REMARKS
01-01	M1-94	875-80 RT	Archer Ave	84.0 X 21.0		2			12.25		1	2	
01-02	D10-3	889+35 RT	MILE 135	12.0 X 48.0	1				4.00		1	1	
02-01	D10-3	112+18 RT	MILE 136	12.0 X 48.0	1				4.00		1	1	
02-02	R3-4B	117+70 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
02-03	R3-4B	118+30 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
02-06	E5-1A	144+12 RT	EXIT 136, ARROW	90.0 X 60.0			2		37.50		1	2	
02-07	W5-52L		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
02-08	W5-52R		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
02-09	W4-1R	156+63 RT	THRU TRAFFIC MERGE RIGHT SYMBOL	48.0 X 48.0			1			16.00	1	1	
03-01	D10-3	164+99 RT	MILE 137	12.0 X 48.0	1				4.00		1	1	
03-02	J4-2	179+78 RT	NORTH IH 39, NORTH US 51	72.0 X 54.0			2		27.00		1	2	
03-03	R3-4B	184+70 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
03-04	R3-4B	185+30 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
03-05	R4-3	200+50 LT	SLOWER TRAFFIC KEEP RIGHT	48.0 X 60.0			1		20.00		1	1	
03-07	D10-3	217+80 RT	MILE 138	12.0 X 48.0	1				4.00		1	1	
04-03	R3-4B	265+70 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
04-04	12-2	266+31 RT	Portage County	84.0 X 21.0		2			12.25		1	2	
04-05	R3-4B	266+30 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
04-06	D10-3	217+80 RT	MILE 139	12.0 X 48.0	1				4.00		1	1	
04-09	E5-1A	310+12 RT	EXIT 139, ARROW	90.0 X 60.0			2		37.50		1	2	
05-01	W4-1R	322+79 RT	THRU TRAFFIC MERGE RIGHT SYMBOL	48.0 X 48.0			2			16.00	1	2	
05-02	D10-3	324+40 RT	MILE 140	12.0 X 48.0	1				4.00		1	1	
05-03	J4-2	343+94 RT	NORTH IH 39, NORTH US 51	72.0 X 54.0			2		27.00		1	2	
05-04	R3-4B	347+70 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
05-05	R3-4B	348+30 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
06-01	D10-3	377+20 RT	MILE 141	12.0 X 48.0	1				4.00		1	1	
06-02	M1-94	423+88 RT	3rd Avenue	66.0 X 21.0		2			9.63				
06-03	D10-3	430+00 RT	MILE 142	12.0 X 48.0	1				4.00		1	1	
	W5-52R		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
	W5-52L		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
07-01	R3-4B	471+70 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
07-02	R3-4B	472+30 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
07-03	D10-3	482+80 RT	MILE 143	12.0 X 48.0	1				4.00		1	1	
07-06	E5-1A	493+99 RT	EXIT 143, ARROW	90.0 X 60.0			2		37.50		<u>·</u> 1	2	
07-07	W4-1R	512+98 RT	THRU TRAFFIC MERGE RIGHT SYMBOL	48.0 X 48.0			2			16.00	1	2	
J. J.		3.2.30 111		/			_			. 5.00	•	<del>-</del>	

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#### **SIGNS TYPE II**

						POSTS WOOD 4" X 6"			637.2210 SIGNS	637.2230 SIGNS	638.2602 REMOVING	638.3000 REMOVING	
					634 0612	634.0614		634 0618	TYPE II	TYPE II	SIGNS	SMALL SIGN	
SIGN	SIGN				12-FT	14-FT	16-FT	18-FT	REFLECTIVE H	REFLECTIVE F	TYPE II	SUPPORTS	
NO.	CODE	STATION	MESSAGE	SIZE	EACH	EACH	EACH	EACH	SF	SF	EACH	EACH	REMARKS
00.04													
08-01	J4-2	527+94 RT	NORTH IH 39, NORTH US 51	72.0 X 54.0			2		27.00		1	2	
08-02	R3-4B	534+00 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
08-03	R3-4B	534+60 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	11	
08-04	D10-3	535+60 RT	MILE 144	12.0 X 48.0	1				4.00		1	1	
	W11-57A	550+13 RT	WATCH FOR LOW FLYING PLANES	48.0 X 48.0			2			16.00	1	1	
08-07	R4-3	555+94 LT	SLOWER TRAFFIC KEEP RIGHT	48.0 X 60.0			1		20.00		1	1	
80-80	R8-7	560+16 RT	EMERGENCY STOPPING ONLY	48.0 X 36.0		1			12.00		1	1	
09-01	D10-3	587+40 RT	MILE 145	12.0 X 48.0	1				4.00		1	1	
09-02	W8-71	605+57 RT	WATCH FOR DUST OVER ROAD	48.0 X 48.0			2			16.00	1	1	
09-03	W5-52L		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
09-04	W5-52R		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
09-05	D10-3	640+20 RT	MILE 146	12.0 X 48.0	1				4.00		1	1	
10-01	R3-4B	672+20 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
10-02	R3-4B	672+80 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
10-03	W5-52R		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
10-04	W5-52L		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
10-05	D10-3	693+00 RT	MILE 147	12.0 X 48.0	1				4.00		1	1	
10-06	M1-94	712+10 RT	Coddington Rd	108.0 X 21.0	2				15.75		1	2	
11-01	W5-52R		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	_
11-02	W5-52L		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
11-03	D10-3	745+80 RT	MILE 148	12.0 X 48.0	1				4.00		1	1	
11-04	R3-4B	753+45 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
11-05	R3-4B	754+05 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
11-06	W5-52R		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
11-07	W5-52L		BRIDGE HASH MARKS	18.0 X 54.0	1					6.75	1	1	
11-08	D10-3	798+60 RT	MILE 149	12.0 X 48.0	1				4.00		1	1	
12-01	R3-4B	829+80 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
12-02	R3-4B	830+40 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
12-03	D10-3	851+40 RT	MILE 150	12.0 X 48.0	1				4.00		1	1	
12-06	M1-94	870+60 RT	Birch Dr	66.0 X 21.0		2			9.63				
13-02	D10-3	904+20 RT	MILE 151	12.0 X 48.0	1				4.00		1	1	
13-03	R3-4B	908+70 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
13-04	R3-4B	909+30 LT	NO U-TURN SYMBOL/MAINTENANCE AND POLICE VEHICLES	36.0 X 48.0			1		12.00		1	1	
				22.2 % .3.0			•				•	-	

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#### **SIGNS TYPE II**

					POSTS WOOD 4" X6"			637.2210 SIGNS	637.2230 SIGNS	638.2602 REMOVING	638.3000 REMOVING		
					634.0612	634.0614	634.0616	634.0618	TYPE II	TYPE II	SIGNS	SMALL SIGN	
SIGN	SIGN				12-FT	14-FT	16-FT	18-FT	REFLECTIVE H	REFLECTIVE F	TYPE II	SUPPORTS	
NO.	CODE	STATION	MESSAGE	SIZE	EACH	EACH	EACH	EACH	SF	SF	EACH	EACH	REMARKS
44.04	WO 4	4010DL: 00 DT	OTOD AUGAD	00 0 V 00 0			4			0.00	4	4	
14-01	W3-1	43'CB'+00 RT	STOP AHEAD	36.0 X 36.0			1			9.00	1	1	
14-02	J2-2	45'CB'+50 RT	NORTH IH 39, NORTH US 51, ARROWS	48.0 X 57.0		1			19.00		1	1	
14-03	D1-3	50'CB'+50 RT	Plainfield (ARROW), Nekoosa, Wis Rapids (ARROW)	102.0 X 48.0		2			34.00		1	2	
14-04	R5-1A	50'CB'+50 RT	WRONG WAY	36.0 X 24.0			1		6.00				
14-05	R5-1A	50'CB'+50 LT	WRONG WAY	36.0 X 24.0			1		6.00				
14-06			DO NOT ENTER & WRONG WAY								1	1	
14-07	W12-1D		DOUBLE DIAGONAL ARROW	24.0 X 24.0	1					4.00	1	1	
14-08	R1-1		STOP	36.0 X 36.0		1			7.46		1	1	
14-09	J3-2		NORTH STH 73, SOUTH STH 73, ARROWS	48.0 X 57.0		1			19.00		1	1	
14-10	R5-1		DO NOT ENTER	36.0 X 36.0					9.00				ATTACH TO 14-09
14-11	R1-1		STOP	36.0 X 36.0			1		7.46		1	1	
14-12	R5-1C		DO NOT ENTER	30.0 X 30.0					6.26				ATTACH TO 14-11
14-13	R1-1		STOP	36.0 X 36.0			1		7.46		1	1	
14-14	R6-3		DIVIDED HIGHWAY CROSSING	30.0 X 24.0					5.00		1		ATTACH TO 14-13
14-15	R6-2L		ONE WAY (LT ARROW)	24.0 X 30.0					5.00				ATTACH TO 14-13
14-16	R5-57		PEDESTRIANS ETC, PROHIBITED	36.0 X 36.0			1		9.00		1	1	
15-01		112'GB'+00 RT	STOP AHEAD	36.0 X 36.0			1			9.00	1	1	
15-02			Almond (ARROW) & WRONG WAY				· 				1	2	
15-03			WRONG WAY								1	<u></u>	
15-03		117'GB'+00 RT	ALMOND (ARROW)	78.0 X 21.0		1			11.38				
15-04		117'GB'+00 RT	WRONG WAY	36.0 X 24.0		ı			6.00				
							1						
15-06		117'GB'+00 LT	WRONG WAY	36.0 X 24.0			1		6.00				
15-07	J13-1		CTH D, ARROW	24.0 X 45.0			1		7.50		1	1	
	W12-1D		DOUBLE DIAGONAL ARROW	24.0 X 24.0	1					4.00	1	1	
15-09	R1-1		STOP	36.0 X 36.0			1		7.46		1	1	
15-10	R5-1		DO NOT ENTER	30.0 X 30.0			1		6.26		1	1	
15-11	R1-1		STOP	36.0 X 36.0		1			7.46		1	1	
15-12	R6-1L		ONE WAY (LT ARROW)	36.0 X 12.0					3.00		1		ATTACH TO 15-11
15-13	R6-1R		ONE WAY (RT ARROW)	36.0 X 12.0					3.00				ATTACH TO 15-11
15-14	R5-1		DO NOT ENTER	30.0 X 30.0		11			6.26				
15-15	R1-1		STOP	36.0 X 36.0			1		7.46		1	1	
15-16	R5-1C		DO NOT ENTER	30.0 X 30.0			1		6.26		1	1	ATTACH TO 15-15
15-17	R5-57		PEDESTRIANS ETC, PROHIBITED	36.0 X 36.0		1			9.00		1	1	

CONTINUED ON NEXT PAGE

#### **SIGNS TYPE II**

					POSTS WOOD  4" X 6"			637.2210 SIGNS	637.2230 SIGNS	638.2602 REMOVING	638.3000 REMOVING		
					634.0612	634.0614	634.0616	634.0618	TYPE II	TYPE II	SIGNS	SMALL SIGN	
SIGN	SIGN				12-FT	14-FT	16-FT	18-FT	REFLECTIVE H	REFLECTIVE F	TYPE II	SUPPORTS	
NO.	CODE	STATION	MESSAGE	SIZE	EACH	EACH	EACH	EACH	SF	SF	EACH	EACH	REMARKS
16-01	W3-1	93'MB'+50 RT	STOP AHEAD	36.0 X 36.0				1		9.00	1	2	
16-02	M1-87	96'MB'+00 RT	(BINOCULARS) WILDLIFE VIEWING AREA	18.0 X 36.0			1		4.50		1	1	
16-03	M1-88A	96'MB'+00 RT	ARROW	12.0 X 12.0					1.00		1		ATTACH TO 16-02
16-04	D1-2	101'MB'+00 RT	Wis Rapids (ARROW), Bancroft (ARROW)	102.0 X 36.0		2			25.50		1	2	
16-05	R5-1A	101'MB'+00 RT	WRONG WAY	36.0 X 24.0		1			6.00		1		
16-06	R5-1A	101'MB'+00 LT	WRONG WAY	36.0 X 24.0		1			6.00		1	1	
16-07	J13-1		CTH W, ARROW	24.0 X 45.0		1			7.50		1	1	
16-08	W12-1D		DOUBLE DIAGONAL ARROW	24.0 X 24.0	1					4.00	1	1	
16-09	R1-1		STOP	36.0 X 36.0		1			7.46		1	1	
16-10	R5-1C		DO NOT ENTER	30.0 X 30.0					6.26		1	1	ATTACH TO 16-09
16-11	R1-1		STOP	36.0 X 36.0		1			7.46		1	1	
16-12	R5-1C		DO NOT ENTER	30.0 X 30.0					6.26		1	1	ATTACH TO 16-11
16-13	R1-1		STOP	36.0 X 36.0		1			7.46		1	1	
16-14	R5-1C		DO NOT ENTER	30.0 X 30.0					6.26		1	1	ATTACH TO 16-13
16-15	R5-57		PEDESTRIANS ETC, PROHIBITED	36.0 X 36.0		1			9.00		1	1	
				TOTALS	34	25	58	1	929.10	200.00	103	113	

NOTE: SEE RAMP GATE SUMMARY AND RAMP GATE LOCATION DETAILS FOR TYPE II REFLECTIVE H FOLDING SIGNS R11-54F (180 SF) AND 4"x6" 16-FT WOOD POSTS (6 EACH)

#### SIGNS TYPE I

					635.0300	637.1220	638.2601	
					SIGN SUPPORTS	SIGNS	REMOVING	
					REPLACING BASE	TYPE I	SIGNS	
SIGN	SIGN				CONNECTION BOLTS	REFLECTIVE SH	TYPE I	
NO.	CODE	STATION	MESSAGE	SIZE	EACH	SF	EACH	REMARKS
01-03	E1-1A	911+26 RT	STH 73, Plainfield, Wis Rapids, 1 MILE	204.0 X 162.0	1	229.50	1	
01-04	E1-5	911+26 RT	EXIT 136	120.0 X 30.0		25.00	1	ATTACH TO SIGN 01-02
02-04	E4-1A	134+09 RT	STH 73, Plainfield, Wis Rapids, Arrow	240.0 X 132.0	1	220.00	1	
02-05	E1-5	134+09 RT	EXIT 136	120.0 X 30.0		25.00	1	ATTACH TO SIGN 02-04
03-06	D2-3	203+13 RT	Almond 8, Stevens Point 22, Wausau 55	228.0 X 84.0	1	133.00	1	
04-01	E1-1A	252+50 RT	CTH D, Almond 1 MILE	156.0 X 120.0	1	130.00	1	
04-02	E1-5	252+50 RT	EXIT 139	120.0 X 30.0		25.00	1	ATTACH TO SIGN 04-01
04-07	E4-1A	303+46 RT	CTH D, Almond, Arrow	192.0 X 90.0	1	120.00	1	
04-08	E1-5	303+46 RT	EXIT 139	120.0 X 30.0		25.00	1	ATTACH TO SIGN 04-07
06-04	E2-1	437+62 RT	NEXT EXIT 8 MILES	156.0 X 24.0		26.00	1	ATTACH TO SIGN 06-04
06-05	E1-1A	437+62 RT	CTH W, Bancroft, Wis Rapids, 1 MILE	204.0 X 156.0	1	221.00	1	
06-06	E1-5	437+62 RT	EXIT 143	120.0 X 30.0		25.00	1	ATTACH TO SIGN 06-04
07-04	E4-1A	487+32 RT	CTH W, Bancroft, Wis Rapids, Arrow	240.0 X 126.0	1	210.00	1	
07-05	E1-5	487+32 RT	EXIT 143	120.0 X 30.0		25.00	1	ATTACH TO SIGN 07-04
08-07	D2-3	550+13 RT	Stevens Point 15, Wausau 48, Minocqua 117	228.0 X 84.0	1	133.00	1	
12-04	E1-1A	861+96 RT	STH 54, BUS 51, Plover, Wis Rapids 1 1/4 MILES	204.0 X 174.0	1	246.50	1	
12-05	E1-5	861+96 RT	EXIT 151	120.0 X 30.0		25.00	1	ATTACH SIGN TO 12-04
13-01	E3-1	889+94 RT	Waupaca, EXIT 151	132.0 X 60.0	1	55.00	1	
13-05	E4-1A	926+40 RT	STH 54, BUS 51, Plover, Wis Rapids, Arrow	240.0 X 144.0	1	240.00	1	
13-06	E1-5	926+40 RT	EXIT 151	120.0 X 30.0		25.00	1	ATTACH SIGN TO 13-05
				TOTALS	11	2164.00	20	=
				IOIALO	1 1	2107.00	20	

#### PAVEMENT MARKING

STATION - STATION	LOCATION	646.0106 EPOXY 4-INCH EDGE LINE WHITE LF	646.0106 EPOXY 4-INCH EDGE LINE YELLOW LF	646.0106 EPOXY 4-INCH DOTTED EDGE LINE WHITE LF	646.0881.S GROOVED WET REFLECTIVE TAPE 4-INCH LANE LINE WHITE LF	646.0883.S GROOVED WET REFLECTIVE TAPE 8-INCH CHANNELIZING WHITE LF	647.0746 DIAGONAL EPOXY 24-INCH WHITE LF	649.0200 TEMPORARY REFLECTIVE PAINT 4-INCH LANE LINE WHITE LF	649.0400 TEMPORARY PAVEMENT MARKING REMOVABLE TAPE 4-INCH LF
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	RT	8233							
STA 94+99.60 AHD - STA 137+09	RT	4209							
STA 143+55 - STH 144+00	RT	45							
STA 163+00 - STA 164+00	RT	100							
STA 175+00 - STA 232+89.45 BK	RT	5789							
STA 233+89.45 AHD - STA 303+51	RT	6962							
STA 309+10 - STA 329+30	RT	2020							
STA 340+00 - STA 487+60	RT	14760							
STA 493+00 - STA 514+50	RT	2150							
STA 522+46 - STA 556+01.64 BK	RT	3356							
STA 555+01.64 AHD - STA 926+15 END PROJECT	RT	37113							
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	LT		8233						
STA 94+99.60 AHD - STA 144+00	LT		4900						
STA 163+00 - STA 232+89.45 BK	LT		6989						
STA 233+89.45 AHD - STA 556+01.64 BK	LT		32212						
STA 555+01.64 AHD - STA 926+15 END PROJECT	LT		37113						
'CB' RAMP		1247	601	104		440	94		
'CA' RAMP		1954	854			1040			
'GB' RAMP		1398	839	48		720	170		
'GA' RAMP		2006	936			1180			
'MB' RAMP		1390	850	43		720	178		
'MA' RAMP		1796	1000			1204			
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	CL				2058			659	
STA 94+99.60 AHD - STA 144+00	CL				1225			392	
STA 163+00 - STA 232+89.45 BK	CL				1747			559	
STA 233+89.45 AHD - STA 556+01.64 BK	CL				8049			2576	
STA 555+01.64 AHD - STA 926+15 END PROJECT	CL				9252			2961	
STA 144+00 - STA 163+00	CL								
STAGE 2, 3, 4, AND 5 TRAFFIC CONTROL									18,720
	SUBTOTALS	94528	94527	195	22331	5304	442	7147	18720
	TOTALS		189250		22331	5304	442	7147	

NOTE: DO NOT APPLY GROOVED WET REFLECTIVE TAPE 4-INCH WHITE LANE LINE ON EXISTING BRIDGE DECKS OR CONCRETE PAVEMENT DO NOT APPLY TEMPORARY REFLECTIVE PAINT 4-INCH ON EXISTING BRIDGE DECKS OR CONCRETE PAVEMENT

#### TRAFFIC CONTROL

	643.0300 DRUMS	643.0715 WARNING LIGHTS TYPE C	643.0800 ARROW BOARDS	643.0900 SIGNS	643.1051 PCMS CELLULAR COMMUNICATIONS	643.3000 DETOUR SIGNS
LOCATION	DAYS	DAYS	DAYS	DAYS	DAYS	DAYS
7 DAYS PRIOR TO START STAGE I	0 288	0 0	0 12	0 24	7 0	0 0
STAGE 2	1131	48	6	30	3	0
STAGE 3	2085	48	6	39	0	0
STAGE 4	1131	48	6	30	3	0
STAGE 5	1131	0	6	30	3	0
WIDE LOAD SIGNING	0	0	0	16	0	0
RAMP CLOSURES	50	0	0	0	24	42
LANE CLOSURES AT GORES	288	0	6	12	0	0
LANE CLOSURE BETWEEN GORES	200	0	0	0	0	0
GORES	240	0	0	0	0	0
UNDISTRIBUTED	1131	48	6	30	0	0
TOTALS	7675	192	48	211	40	42

#### **CONSTRUCTION STAKING**

STATION - STATION	650.8000 RESURFACING REFERENCE LF
STA 842+65 BEGIN PROJECT - STA 924+97.66 BK	8233
STA 94+99.60 AHD - STA 144+00	4900
STA 163+00 - STA STA 232+89.45 BK	6989
STA 233+89.45 AHD - STA 556+01.64 BK	32212
STA 555+01.64 AHD - STA 926+15 END PROJECT	37113
TOTAL	89448

### SAWING ASPHALT

		690.0150	
STATION - STATION	LOCATION	LF	REMARKS
			_
STA 40'CB'+81 - STA 40'CB'+85	'CB' RAMP	32	TRANSVERSE CRACK REPAIR
STA 40'CB'+88 - STA 41'CB'+10	'CB' RAMP	68	TRANSVERSE CRACK REPAIR
STA 140+96 - STA 141+02	DRIVING LANE	36	TRANSVERSE CRACK REPAIR
STA 141+04 - STA 141+08	PASSING LANE	32	TRANSVERSE CRACK REPAIR
	TOTAL	168	

#### SEALING ASPHALTIC PAVEMENT CRACKS

		SPV.0090.01
STATION - STATION	LOCATION	LF
STA 842+65 - STA 926+15	DRIVING AND PASSING LANES	67200
'CB' RAMP		800
'CA' RAMP		1000
'GB' RAMP		800
'GA' RAMP		1000
'MB' RAMP		750
'MA' RAMP		1000
	TOTAL	72550

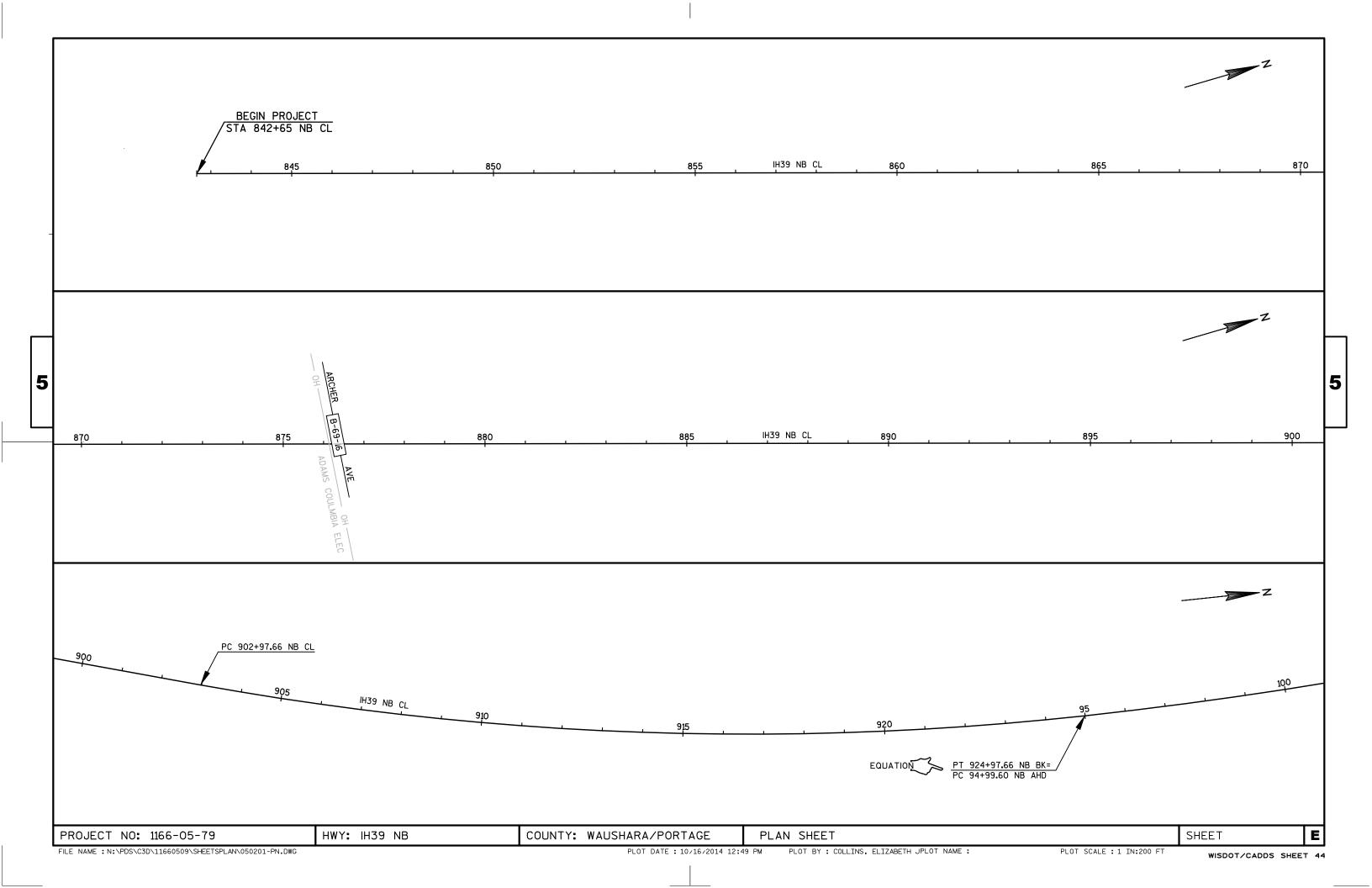
THIS ITEM OF WORK TO BE DONE IN SPRING 2018

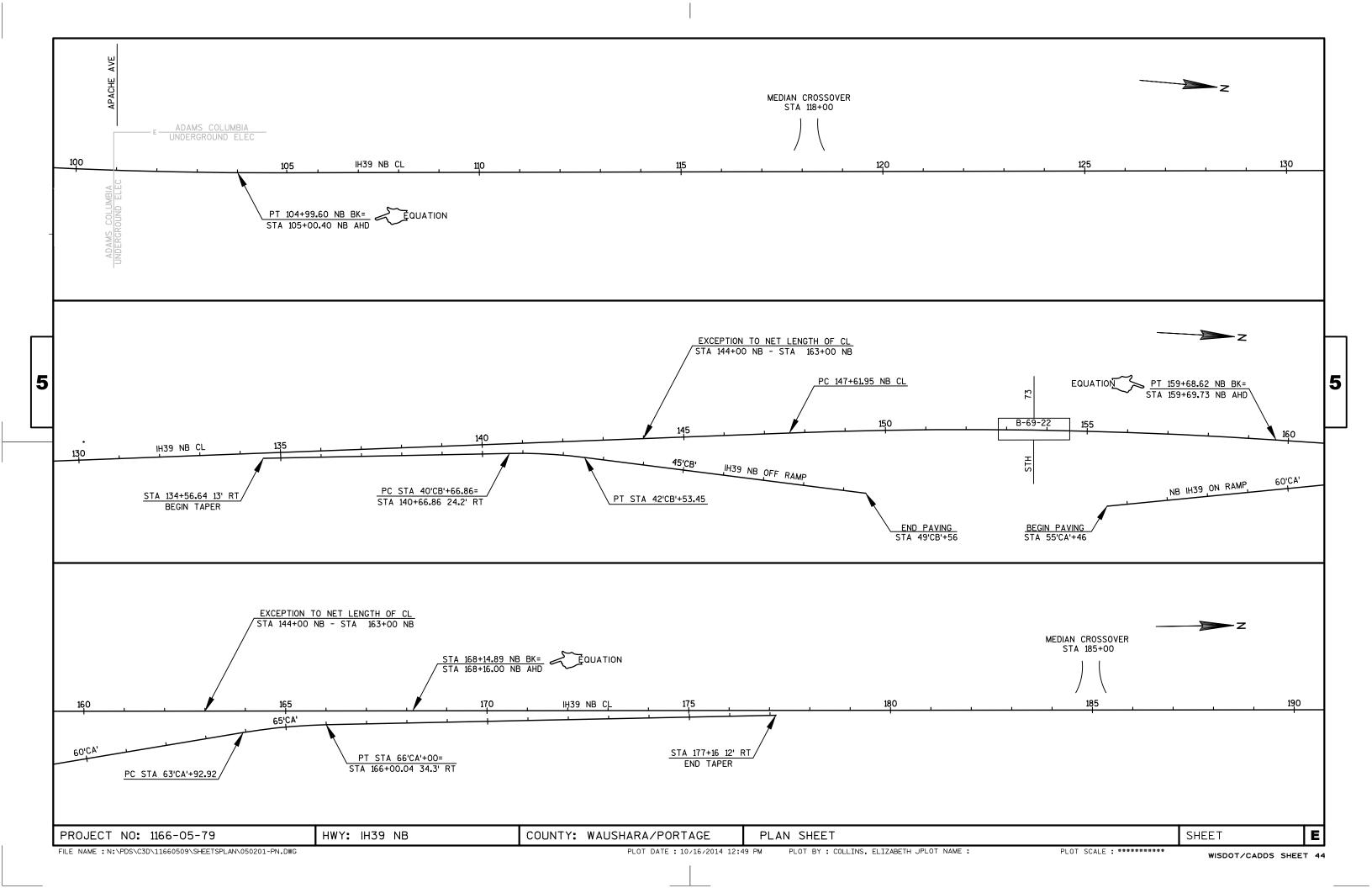
### RAMP GATE SUMMARY

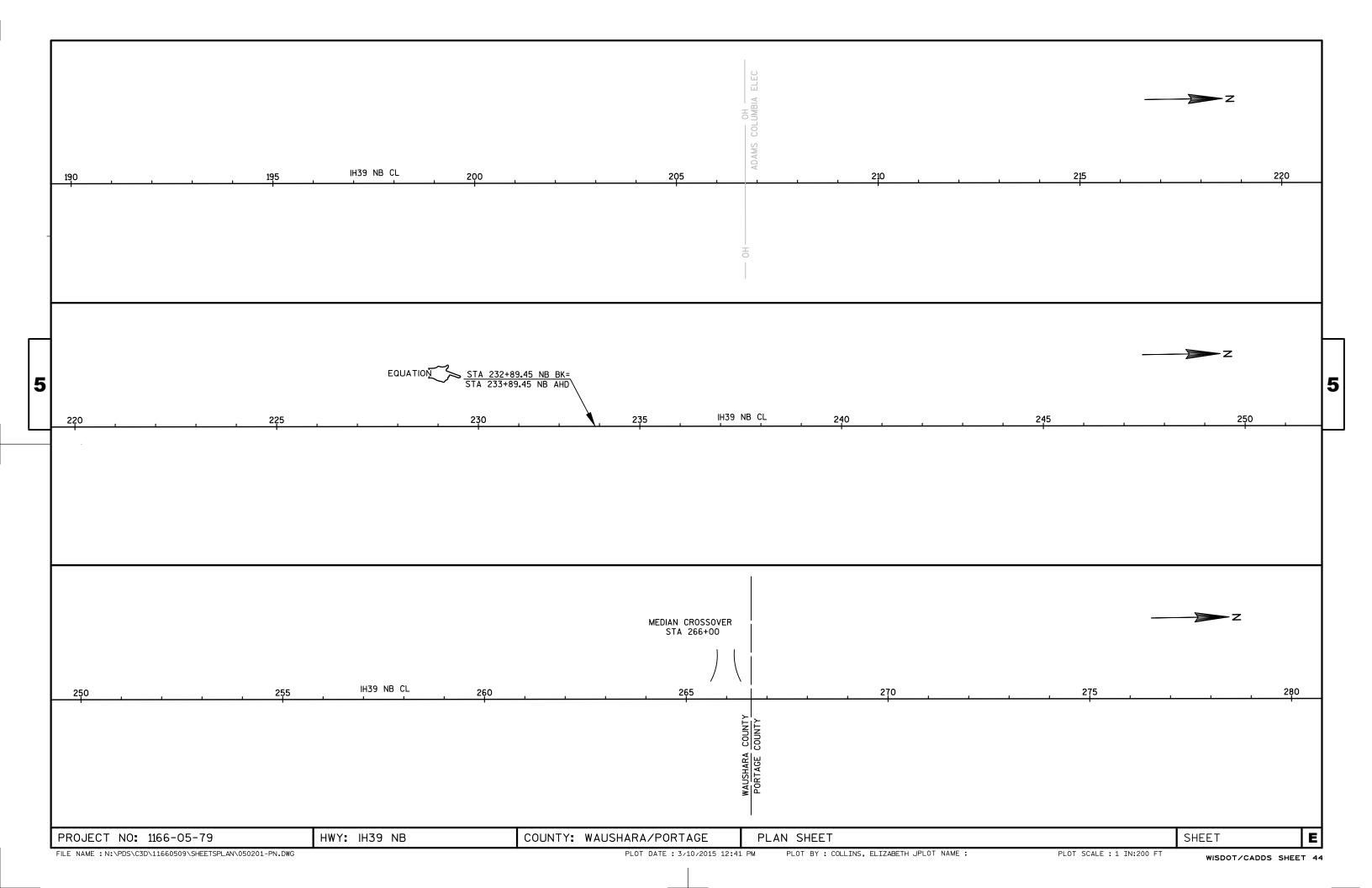
GATE ID	LOCATION	652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH LF	653.0135 PULL BOX STEEL 24X36-INCH EACH	654.0105 CONCRETE BASES TYPE 5 EACH	654.0220 CONCRETE CONTROL CABINET BASE TYPE 10 EACH	657.0100 PEDESTAL BASE EACH	657.0255 TRANSFORMER BASES BEAKAWAY 11 1/2-INCH BOLT CIRCLE EACH	657.0425 TRAFFIC SIGNAL STANDARDS ALUMINUM 15-FT EACH	662.2037.S RAMP CLOSURE GATE SOLAR 37-FT EACH	662.2040.S RAMP CLOSURE GATE SOLAR 40-FT EACH
OATE ID	LOOATION		LAOIT	LAOH	LAOIT	LAON	LAOH	LAGIT	LAOIT	LAOIT
G69-03	'CA' RAMP	20	1	1	1	1	1	1	1	0
G69-04	'CC' RAMP	20	1	1	1	1	1	1	0	1
G49-20	'GA' RAMP	20	1	1	1	1	1	1	0	1
G49-21	'GC' RAMP	20	1	1	1	1	1	1	0	1
G49-22	'MA' RAMP	20	1	1	1	1	1	1	0	1
G49-23	'MC' RAMP	20	1	1	1	1	1	1	0	1
	TOTALS	120	6	6	6	6	6	6	1	5

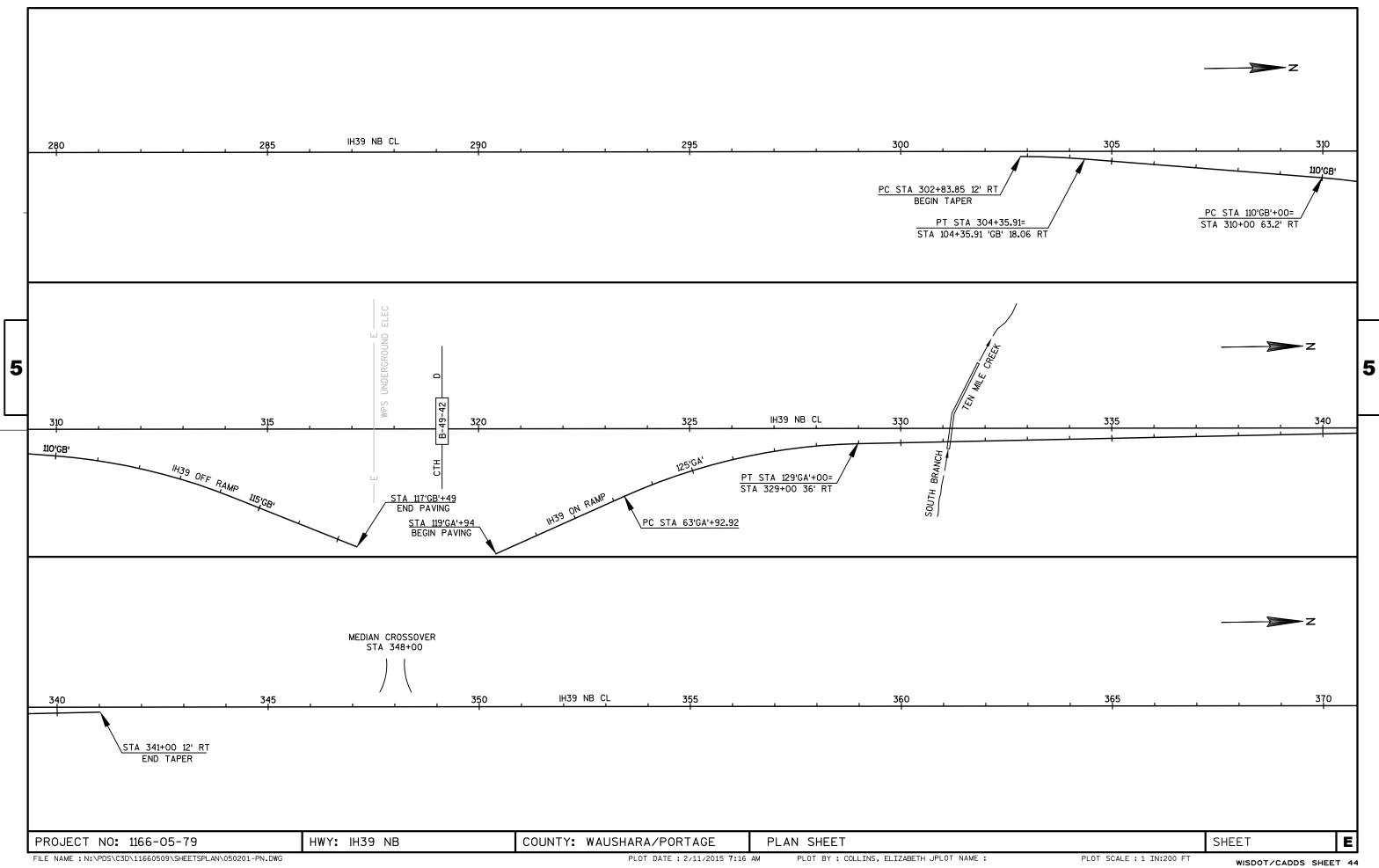
#### RAMP GATE SUMMARY

					637.2215
	662.3037.S	662.3040.S		634.0616	SIGNS TYPE II
	RAMP CLOSURE	RAMP CLOSURE	662.4000.S	POSTS	REFLECTIVE H
	GATE ARMS	GATE ARMS	RAMP CLOSURE	WOOD	FOLDING
	STOCKPILE	STOCKPILE	GATE FLASHERS	4"x6"	R11-54F
	37-FT	40-FT	STOCKPILE	16-FT	48"X30"
LOCATION	EACH	EACH	EACH	EACH	SF
'CA' RAMP				1	30.00
'CC' RAMP				1	30.00
'GA' RAMP				1	30.00
'GC' RAMP				1	30.00
'MA' RAMP				1	30.00
'MC' RAMP				1	30.00
PROJECT	2	2	2		
TOTALS	2	2	2	6	180.00

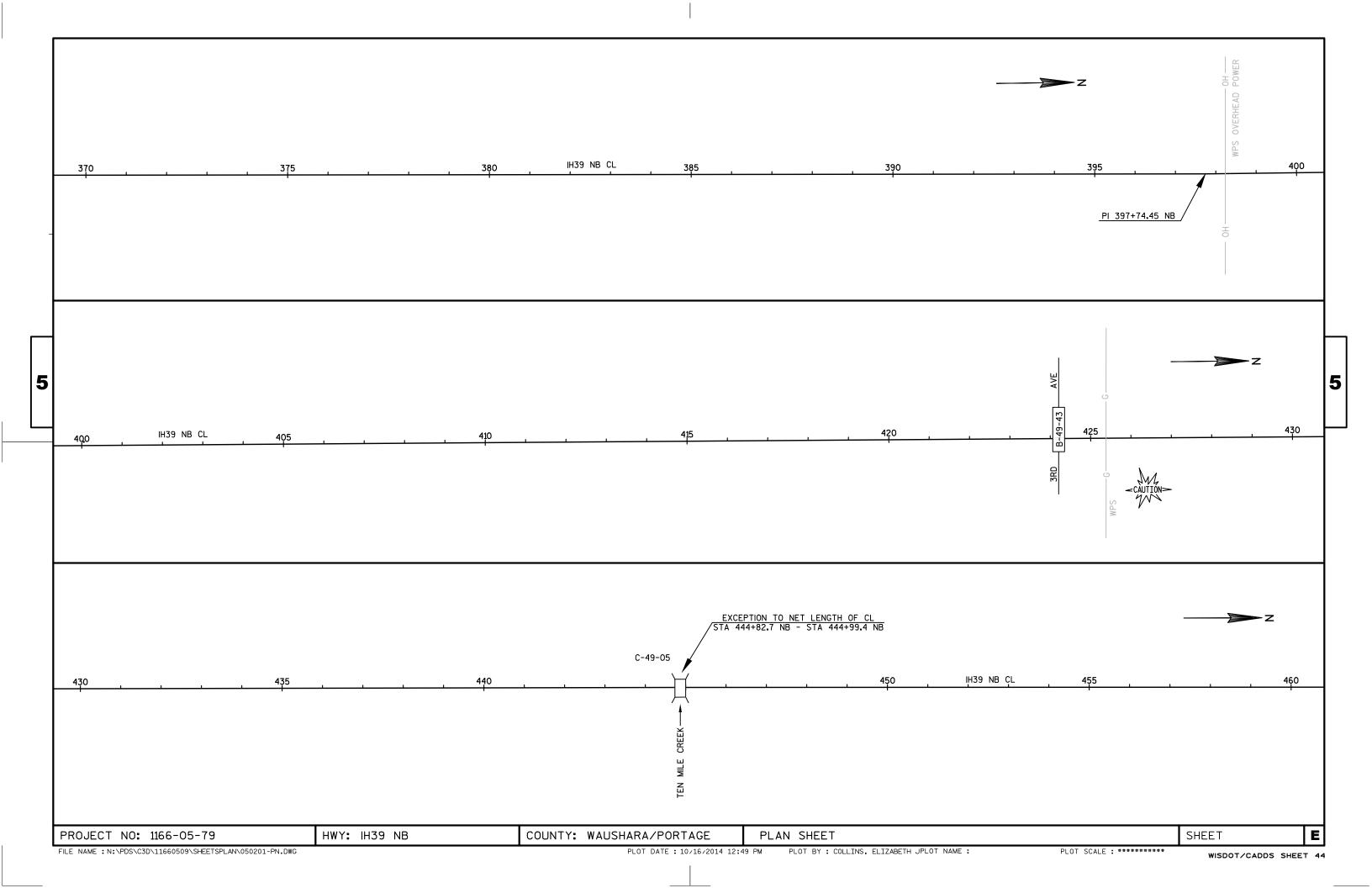


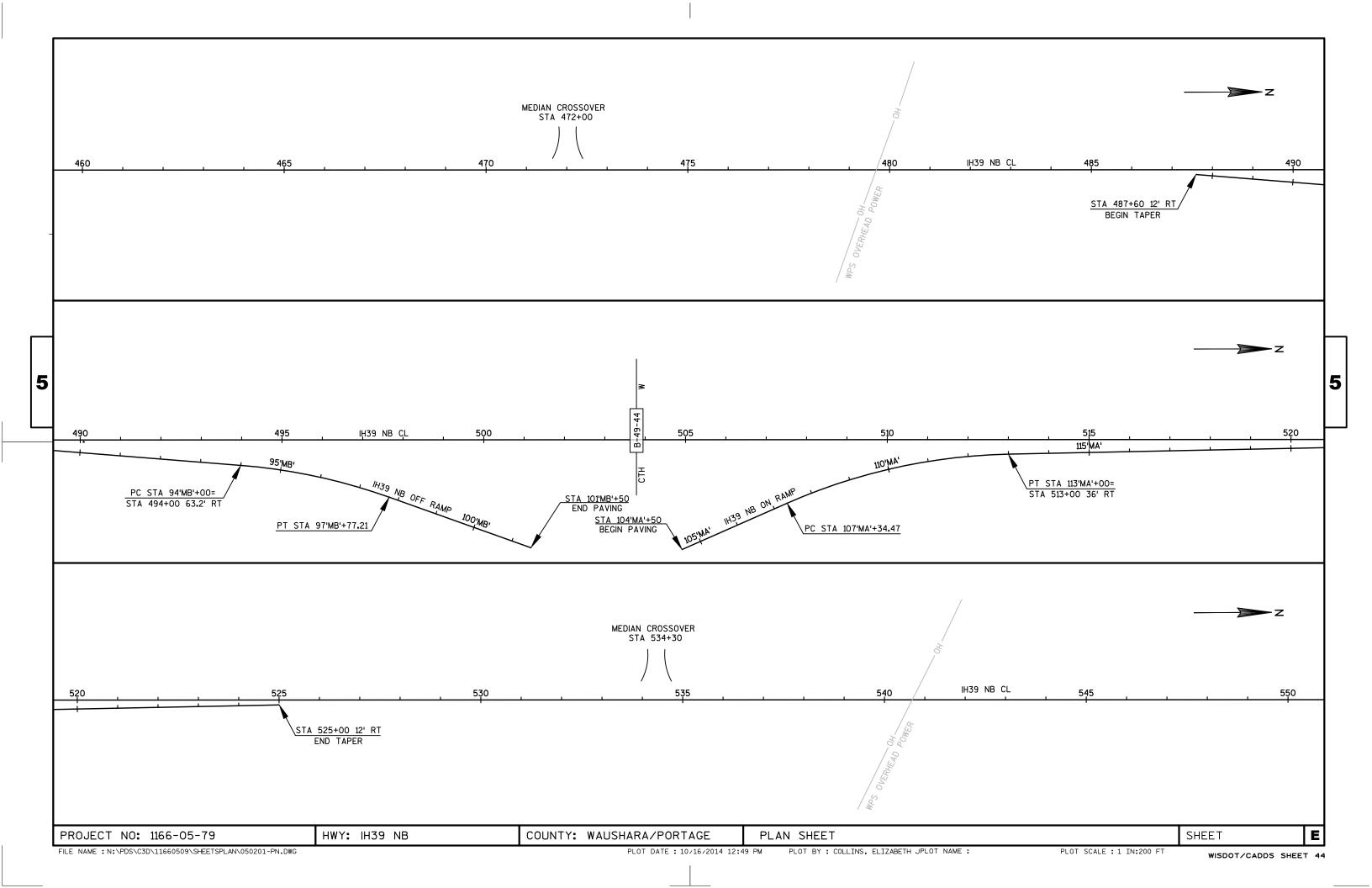


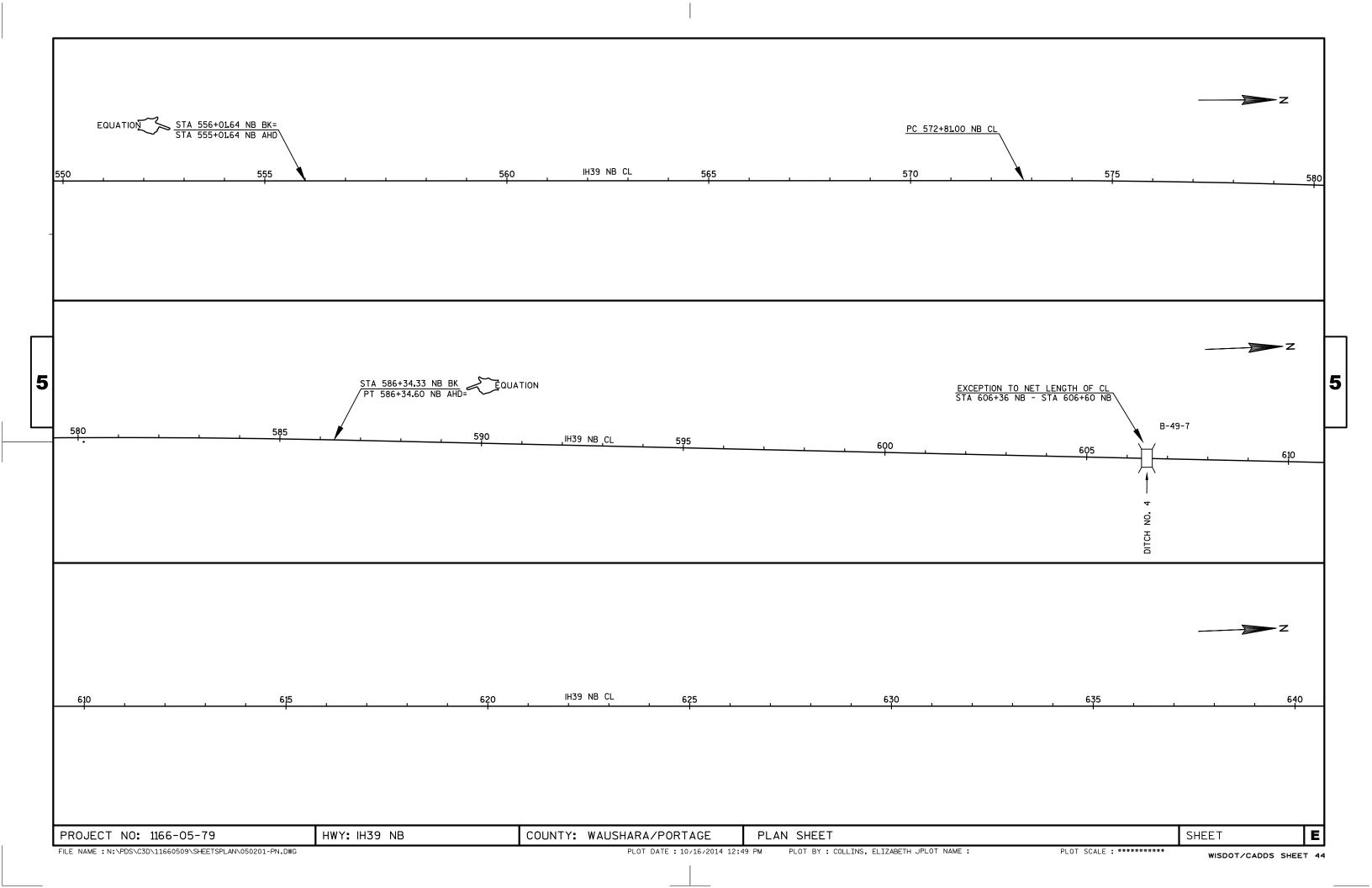


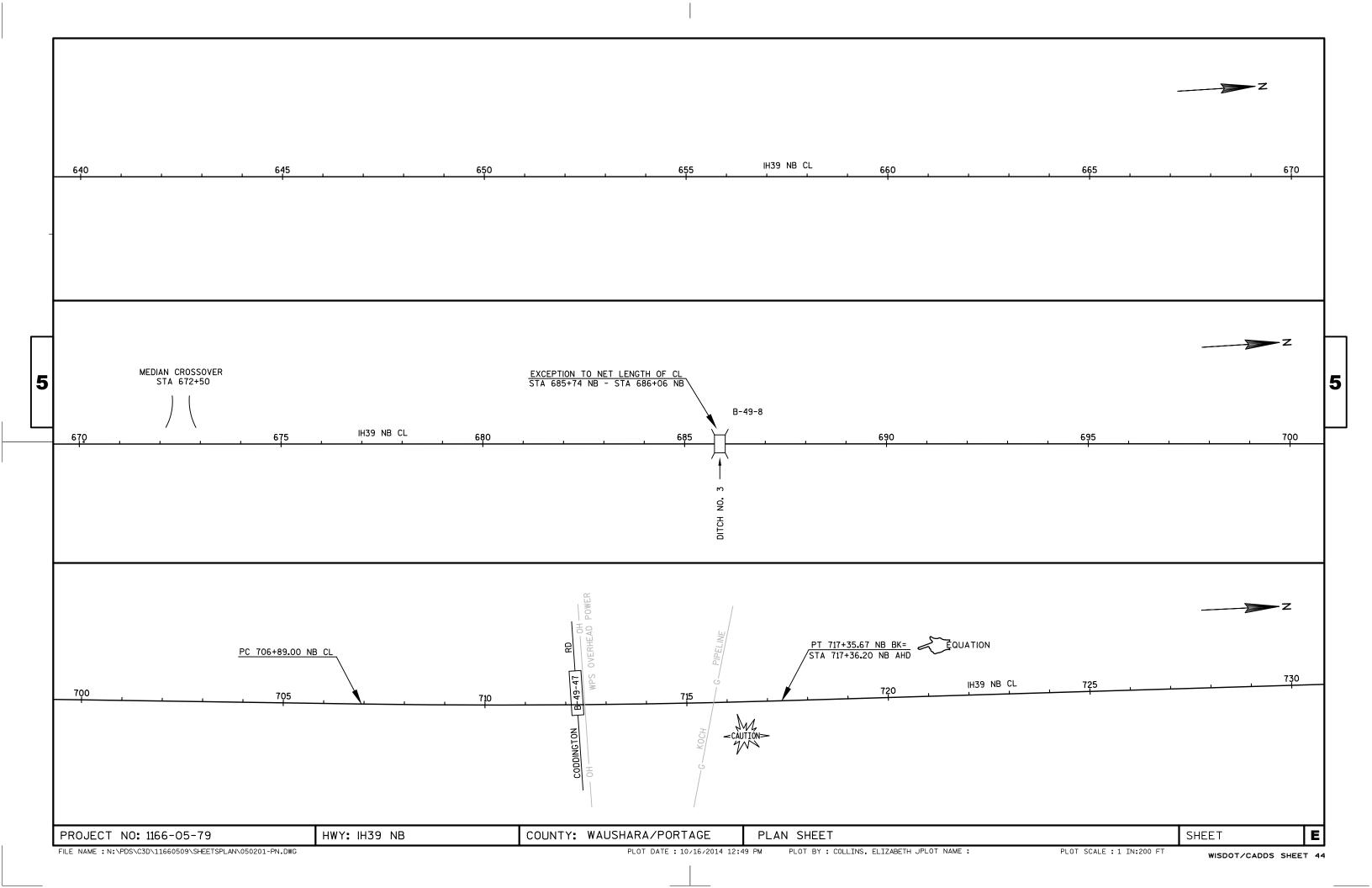


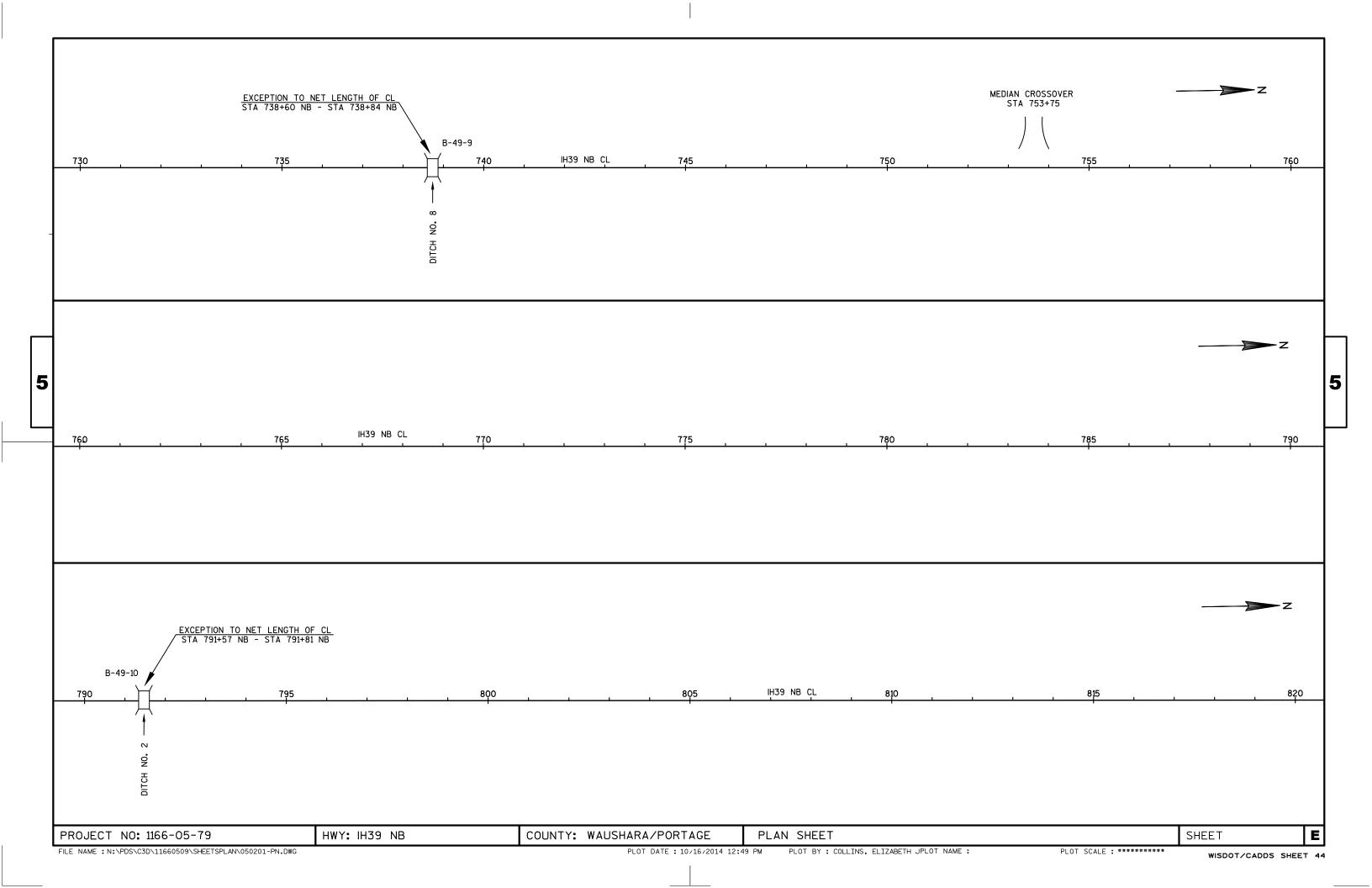
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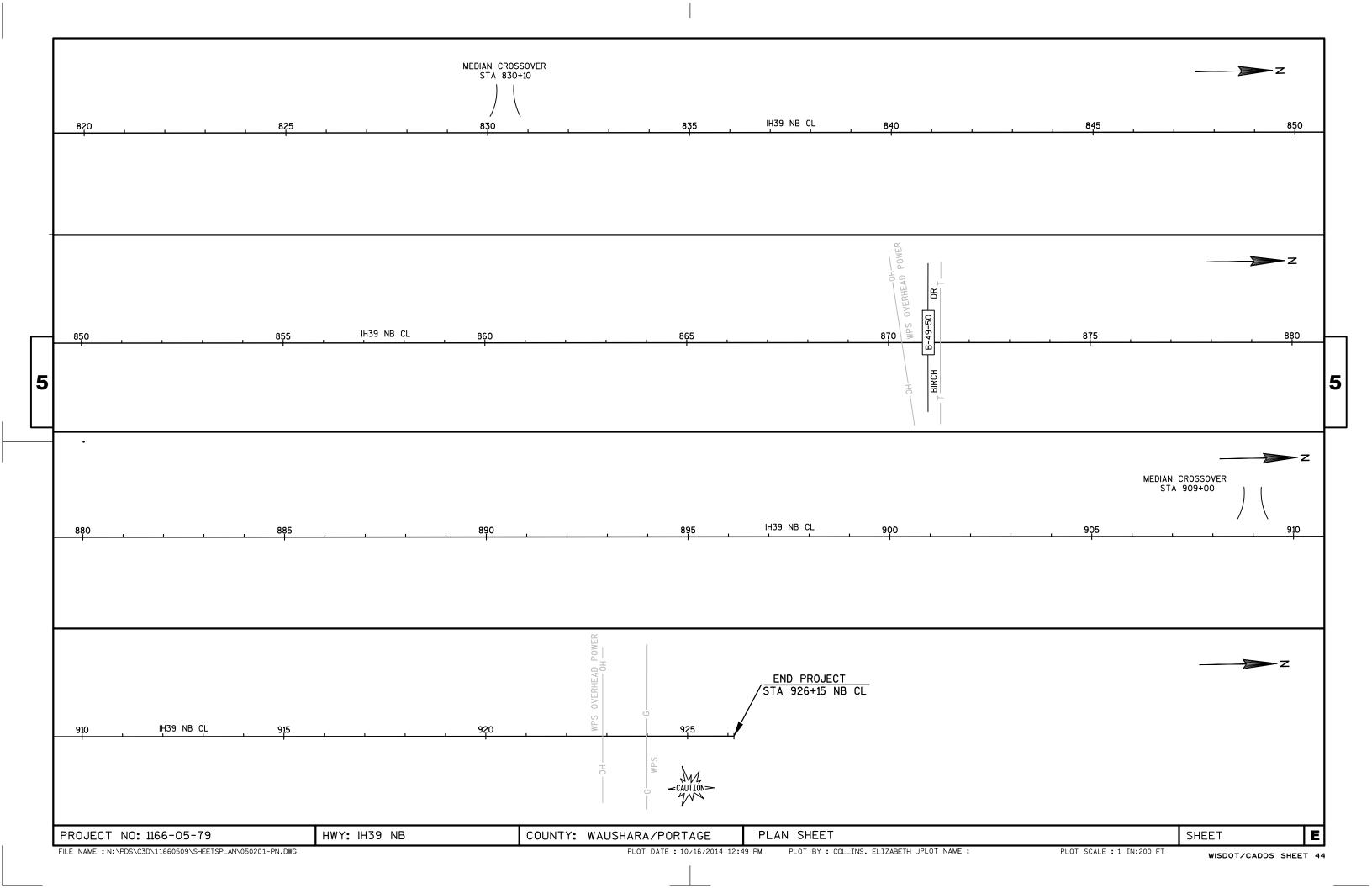










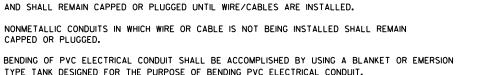


## Standard Detail Drawing List

09B02-09	CONDUI T
09B04-11	PULL BOX
09C02-07	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09C05-10	CONCRETE CONTROL CABINET BASES
12A04-03	STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES & OVERHEAD SIGN SUPPORTS & TRAFFIC SIGNALS
13A05-05A	SHOULDER RUMBLE STRIP, MILLING
13A05-05B	SHOULDER RUMBLE STRIP, MILLING
15A02-09	DELINEATOR POST, DELINEATOR REFLECTOR AND DELINEATOR BRACKET WITH REFLECTIVE SHEETING
15A06-02	DELI NEATOR LAYOUT
15C08-17A	LONGI TUDI NAL MARKI NG (MAI NLI NE)
15C31-02A	PAVEMENT MARKING (RAMPS AND GORES)
15C31-02C	PAVEMENT MARKING FOR PARALLEL ON-RAMP AND PARALLEL OFF-RAMP
15D12-06A	TRAFFIC CONTROL, LANE CLOSURE
15D12-06B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION
15D16-03	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D34-02A	RAMP GATE SOLAR POWER
15D34-02B	RAMP GATE SOLAR POWER
15D34-02C	RAMP GATE SOLAR POWER
15D34-02D	RAMP GATE SOLAR POWER

6

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TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES

SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REIN-STALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY

**GENERAL NOTES** 

AND 36 INCHES MAXIMUM.

OF THE ENGINEER.

CAPPED OR PLUGGED.

MINIMUM AND 36 INCHES MAXIMUM.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

BOTTOM OF ¼" HOLE PVC CONDUIT-CONDUIT TRENCH FOR DRAINAGE NO. 2 COARSE AGGREGATE FILL —1'-0" DIA. OR SQUARE —>

NOTE: INSTALL AT LOCATIONS WHERE METALLIC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

BOTTOM OF

CONDUIT TRENCH

NOTE: INSTALL AT LOCATIONS WHERE PVC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

DRAIN SUMP FOR METALLIC CONDUIT

1'-0" DIA. OR SQUARE ──➤

METALLIC CONDUIT-

1" DIA. X 6"

NIPPLE

NO. 2 COARSE

AGGREGATE FILL

ARROW MARK SHALL BE INSCRIBED IN PAVEMENT SURFACE 1/4" TO 3/8"

DEEP AT EACH LOCATION WHERE CONDUITS ARE PLACED UNDER

**PLAN VIEW** 

ARROW MARK

CONDUIT

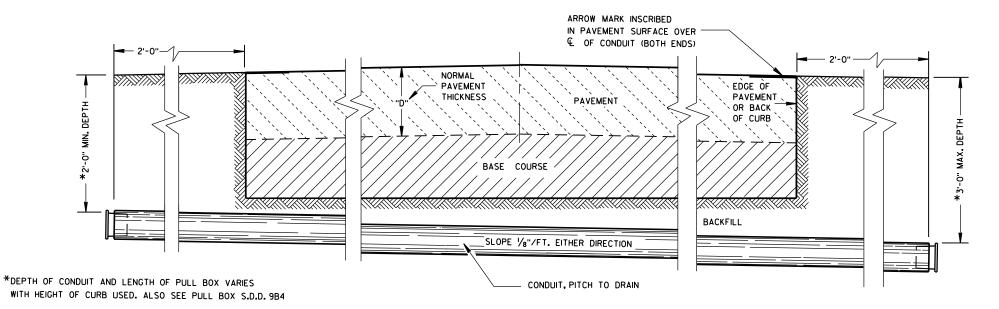
THE PAVEMENT

EDGE OF

PAVEMENT OR BACK

OF CURB

DRAIN SUMP FOR PVC CONDUIT



SIDE ELEVATION DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

CONDUIT

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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**APPROVED** /S/ Ahmet Demirbilek June. 2015 DATE STATE ELECTRICAL ENGINEER

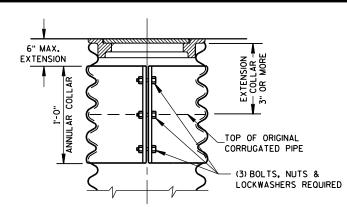
D ဖ  $\Box$ 

6

FHWA

DIMENSION IN INCHES	CORRUGATED STEEL PIPE									
PIPE DIAMETER (INSIDE)	Α	12	12	12	18	18	18	24	24	24
PIPE LENGTH **	В	24	30	36	24	30	36	36	42	48
WALL THICKNESS	C	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 1/4	10 1/4	10 1/4	16 1/4	16 1/4	16 1/4	22 1/4	22 1/4	22 1/4
FRAME	E	14 1/2	14 ½	14 ½	20 ½	20 ½	20 ½	26 ½	26 ½	26 ½
FRAME	F	8 1/2	8 1/2	8 1/2	14 1/2	14 ½	14 1/2	20 ½	20 ½	20 ½
FRAME	G	11 1/2	11 ½	11 1/2	17 1/2	17 1/2	17 1/2	23 ½	23 ½	23 ½
WEIGHT IN POUNDS *									1	
FRAME AND COVER		60	60	60	110	110	110	155	155	155

- \* THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.
- NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.



ELECTRIC

FINAL GRADE

ALL METALLIC CONDUIT

AND THREADED

CUT OPENINGS

THE FIELD

2" PVC PIPE CAP ON BOTH ENDS

WITH 7, 8 1/4" HOLES DRILLED

IN EACH END.

PULL BOX

AS REQUIRED IN

ENDS SHALL BE REAMED

ALL CONDUIT PITCHED

4 TO 8 BRICKS

EQUALLY SPACED

TO DRAIN TO PULL BOXES

2" DRAIN DUCT TO

DITCH OR SEWER

WHEN SPECIFIED

CORRUGATED PIPE EXTENDER

HEAVY DUTY FRAME -

6" MIN.

(TYP.)

AND COVER

WHEN A PULL BOX IS INSTALLED IN CRUSHED

AGGREGATE SHOULDERS, PLACE IT 2-3

2-3 INCHES OF CRUSHED AGGREGATE

NO. 2 COARSE

(SEE SECTION 501

OF THE STANDARD

WIRE AND/OR CABLE.

INSTALL END BELLS (U.L. LISTED FOR

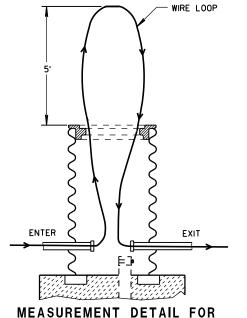
CONDUIT BEFORE INSTALLATION OF

ELECTRICAL USE) ON ALL NONMETALLIC

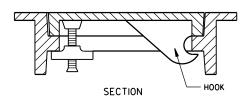
SPECIFICATIONS)

AGGREGATE

INCHES BELOW GRADE AND COVER IT WITH

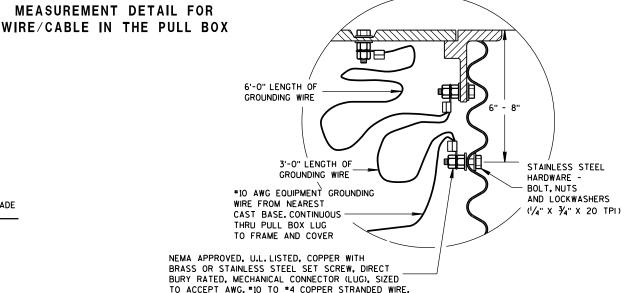


воттом



ALTERNATE COVER (LOCKING)

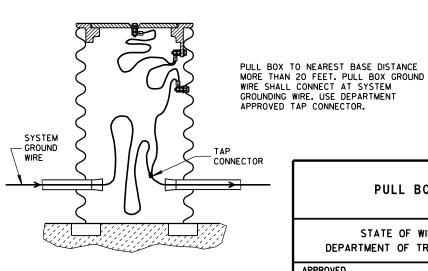
TIGHTENING BAR TYPE



**EQUIPMENT GROUNDING LUG AND** 

**EQUIPMENT GROUNDING LUG AND** LOCATION IN STEEL PULL BOXES

# LOCATION IN STEEL PULL BOXES



PULL BOX

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION 6

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**APPROVED** 

Sept. 2014 /S/ Ahmet Demirbilek DATE STATE ELECTRICAL ENGINEER FHWA

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR TRAFFIC LOADS.

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED

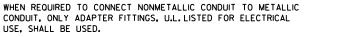
ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED. SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.

**GENERAL NOTES** 

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IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL. THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE.
BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1FOOT OR LESS. A NO. 4 AWG, STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL

BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE

(GROUND ROD) FOR TYPE 1. TYPE 2. TYPE 5. AND TYPE 6 BASES.

**GENERAL NOTES (CONTINUED)** 

ENDS OF CONDUIT INSTALLED BELOW GRADE FOR FUTURE USE SHALL BE

OF CONCRETE BASES BEFORE INSTALLATION OF CABLE OR WIRE.

CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER THE BASE OF THE TYPE 2 AND TYPE 5 BASES THROUGH A LINCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD, ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED. THE 4" "L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND END SHALL NOT BE THREADED.

ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

- 1) THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL BY THE ENGINEER.
- (2) (4) 1" DIA. X 3'-6" ANCHOR RODS.
- (3) (4) 1" DIA. X 5'-0" ANCHOR RODS.
- (4) (6) NO. 6 X 6'-8" BAR STEEL REINFORCEMENT.
- (5) (7) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.
- (6) (4) 1" DIA. X 3'-6" ANCHOR RODS.
- (7) (6) NO.4 X 4'-8" BAR STEEL REINFORCEMENT.
- (8) (5) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.

**GENERAL NOTES** 

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS.

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN AT THE ENTRANCE OF THE BASE.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6 X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

# FORMING DETAIL

1'-8"

a)

- FORM

FORMING SHALL BE

CONCRETE HAS SET

REMOVED AFTER

FORM DEPTH SHALL BE

GRADE ON THE LOWER

SIDE OF BASE

4" MAX.

CONDUIT WITHIN

6" DIA.

ANCHOR RODS SHALL BE

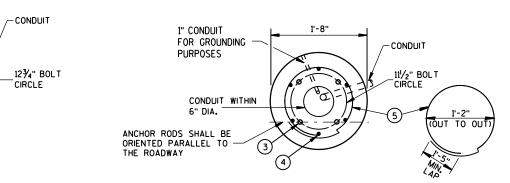
ORIENTED PARALLEL TO

1" CHAMFER ALL AROUND

FORM ALL EXPOSED

CONCRETE, PROVIDE

NO MORE THAN 6" BELOW



QUANTITY

REQUIREMENTS

ARDS OF CONCRETE

APPROX. CUBIC

LBS. OF HOOP

LBS. OF VERTICAL

BAR STEEL

BAR STEEL

CONCRETE BASE TYPE

0.57

23

60

0.40

NONE

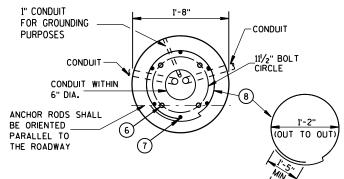
NONE

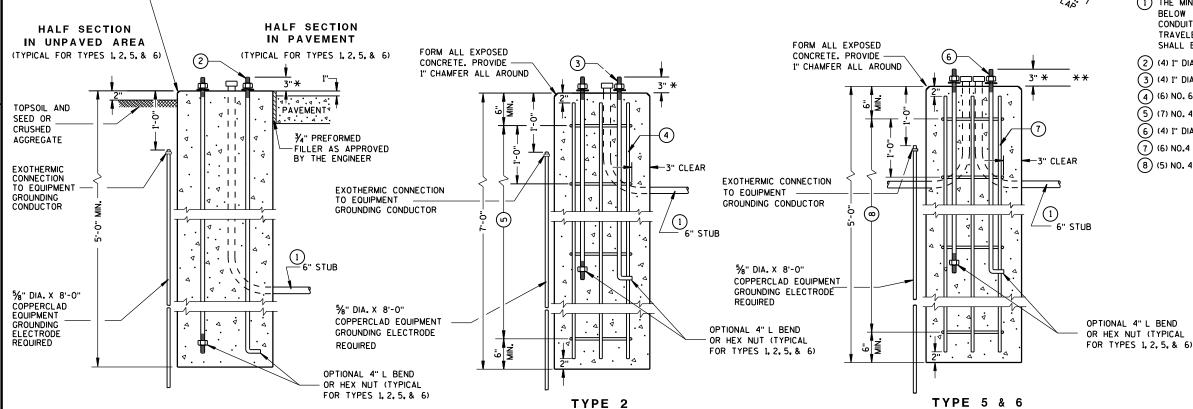
5 & 6

0.40

16

18





**CONCRETE BASES** 

\* ANY ANCHOR ROD PROJECTION SHORTER THAN 2¾" OR LONGER THAN 31/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

\*\* FOR NONBREAKAWAY INSTALLATIONS, 41/2" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

CONCRETE BASES, TYPES 1, 2, 5, & 6

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

**APPROVED** Sept. 2014 /S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER

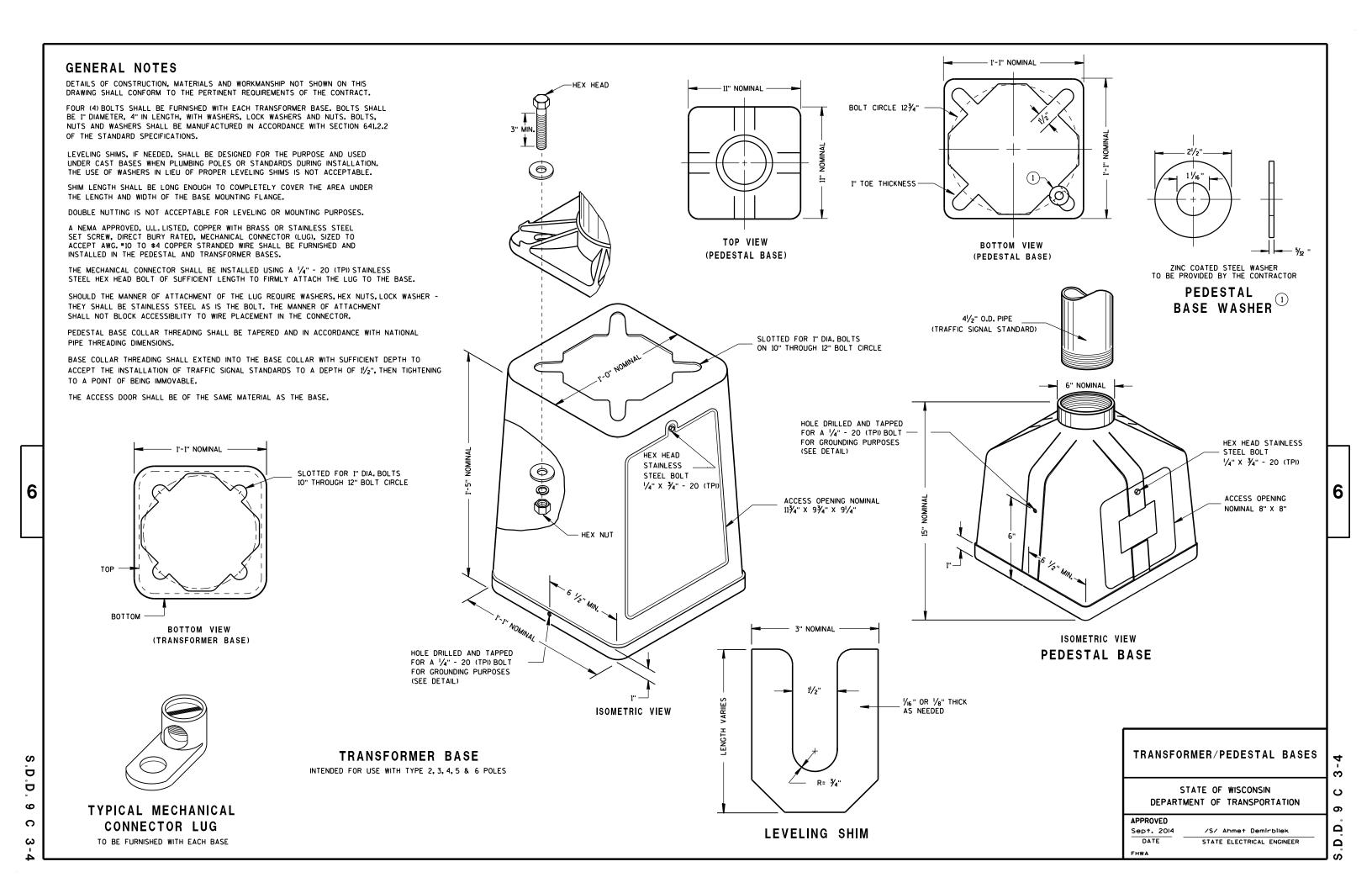
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DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING

SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

**GENERAL NOTES** 

	IMEN	ISION	IS	C.Y. CONCRETE	
Н	ı	7	K	(APPROX.)	
34"	60"	10"	17"	.64	
42"	60"	10"	21"	.93	
42"	72"	12"	21"	1.29	
54"	72"	14"	27"	1.56	
AS SHOWN				.65 <del>X</del>	
	H 34" 42" 42" 54"	H I 34" 60" 42" 60" 42" 72" 54" 72"	H I J 34" 60" 10" 42" 60" 10" 42" 72" 12" 54" 72" 14"	34" 60" 10" 17" 42" 60" 10" 21" 42" 72" 12" 21" 54" 72" 14" 27"	

INCLUDES MAINTENANCE PLATFORM.

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9

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TYPICAL 3'-0" X 3'-0" X 4" THICK MAINTENANCE PLATFORM. LOCATION TO BE DETERMINED IN THE FIELD. COST TO BE -INCLUDED UNDER CONCRETE CONTROL CABINET TYPE 10. EXIT LOCATION OF 11/4" CONDUIT FROM CABINET BASE DEPENDENT UPON LOCATION OF ELECTRIC SERVICE. THE 3" CONDUIT SHALL BE INSTALLED FROM THE CABINET BASE TO THE FIRST (NEAREST) PULL BOX LOCATED AS SHOWN ON THE PLAN A Q ALL CONDUITS WITHIN 6" DIA. CIRCLE

12 ¾" BOLT

4 2" CONDUIT 3" CONDUIT

Δ

4

#### CONDUIT LOCATIONS IN 24" X 36" PULL BOX

(LEADING TO CONTROLLER CABINET BASE TYPE 6, 7, 8 AND 9)

FORM ALL EXPOSED CONCRETE. PROVIDE 1" CHAMFER ALL AROUND ALL CONDUIT SHALL FORM ALL EXPOSED BE INSTALLED WITHIN CONCRETE. PROVIDE 7" X 14" RECTANGLE 1" CHAMFER ALL AROUND LOCATE CONCRETE MAINTENANCE HALF SECTION HALF SECTION OCATE CONCRETE MANY LOVAR OCATE CONCRETE MANY LOVAR OCATE CONCRET (SEE NOTES) IN UNPAVED AREA IN PAVED AREA TOPSOIL AND SIDEWALK SEED OR CRUSHED AGGREGATE ·||| | ¾" PREFORMED FILLER AS 1" CONDUIT - 6" STUB GROUND APPROVED BY THE ENGINEER FOR GROUNDING WIRE LINE ENTRANCE **EXOTHERMIC** 6" STUB CONNECTION THE 3" CONDUIT SHALL BE TO EQUIPMENT APPROX. SPACED 2" MIN. APART TO GROUNDING CONDUCTOR ALLOW FOR PLACEMENT OF 6" STUB-CAPS, BUSHINGS OR COUPLINGS 1 1/4" SERVICE 4 - 6" STUBS SPACED 2" MIN. ENTRANCE APART TO ALLOW FOR PLACEMENT WITH 6" STUB OF CAPS, BUSHING OR COUPLINGS %" DIA. X 8'-0" 2" CONDUIT COPPERCLAD EQUIPMENT COMMUNICATION CABLE 3.0".BASE TYPE 8 & 9 GROUNDING ELECTRODE REQUIRED (ALTERNATE) EXIT LOCATION OF 11/4" CONDUIT 4" L BEND OR FROM CABINET BASE DEPENDENT ONE HEX NUT UPON LOCATION OF ELECTRIC

> TYPE 6,7,8 AND 9 (ISOMETRIC VIEW)

SERVICE.

\* ANY ANCHOR ROD PROJECTION SHORTER THAN 2¾" OR LONGER THAN 31/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

TYPE 10

CONCRETE CONTROL CABINET BASES

CONCRETE CONTROL CABINET BASES

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION 2

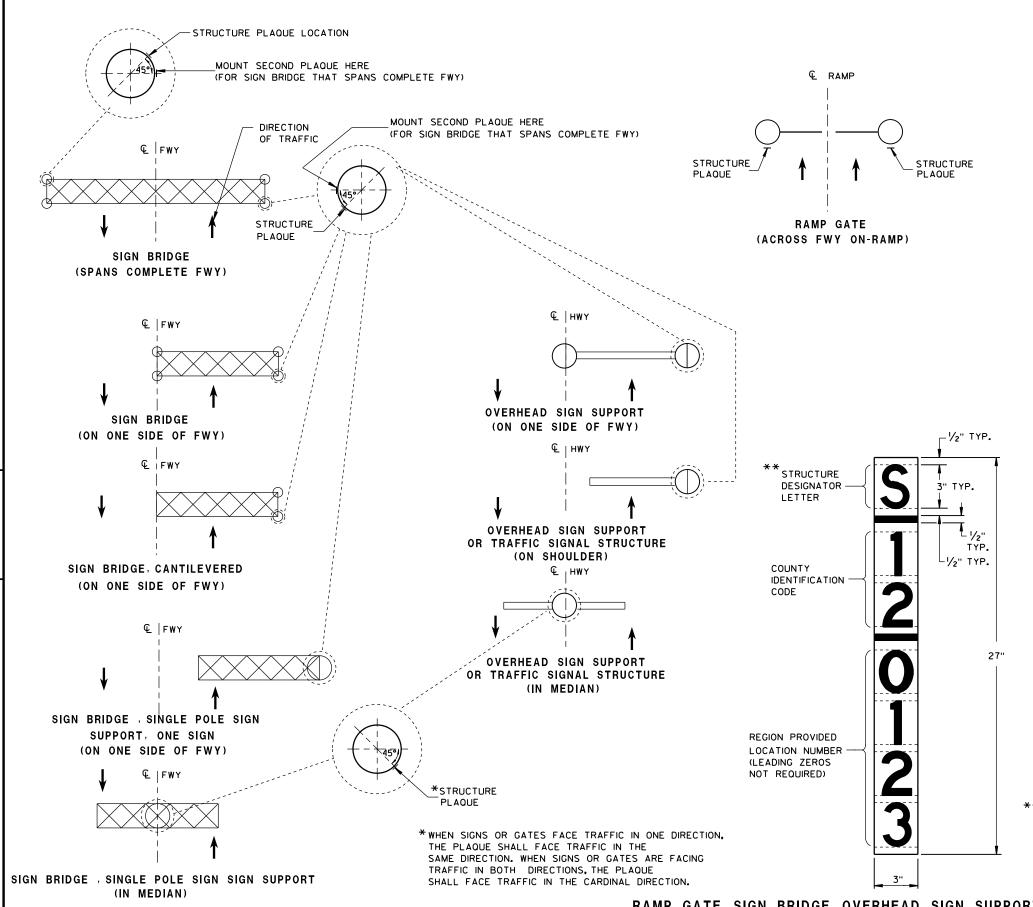
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**APPROVED** 

/S/ Ahmet Demirbilek DATE STATE ELECTRICAL ENGINEER FHWA



3.D.D. 12 A 4-3



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LOCATION OF RAMP GATE, SIGN BRIDGE, OVERHEAD

SIGN SUPPORT & TRAFFIC SIGNAL STRUCTURE PLAQUES

#### GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN IN THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PLAQUES SHALL BE INCIDENTAL TO ALL NEW INSTALLATIONS.

IF THE PROPOSED SIGN BRIDGE OR OVERHEAD SIGN SUPPORT IS REPLACING AN EXISTING SIGN BRIDGE OR OVERHEAD SIGN SUPPORT, A NEW IDENTIFICATION PLAQUE WILL BE REQUIRED.

FASTEN TOP, CENTER AND BOTTOM OF PLAQUE TO POLE OR OTHER LOCATION AS FOLLOWS:

GALVANIZED STEEL SHAFT - 3 STAINLESS STEEL POP RIVETS

A588 STEEL SHAFT - SHIM FOR DRAINAGE WITH STAINLESS WASHERS; FASTEN WITH STAINLESS SELF-TAPPING SCREWS

ALUMINUM SHAFTS - 3 ALUMINUM POP RIVETS

MOUNTING HEIGHT SHALL BE APPROXIMATELY 5.0' ABOVE CURB OR SHOULDER. ADJUST IF IT IS KNOWN THAT REQUIRED TRAFFIC SIGNS WILL OBSTRUCT.

PLAQUE MATERIALS:

BASE - SHEET ALUMINUM, 0.060" THICK.

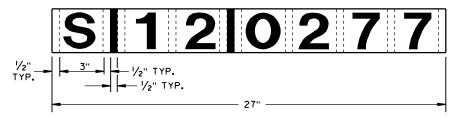
FACE - WHITE, SELF-ADHESIVE VINYL SHEETING, NON-RETROREFLECTIVE

LINES - BLACK, 1/2" WIDE, SELF-ADHESIVE

CHARACTERS:- BLACK, SELF ADHESIVE, SERIES "D", SIZE AS SHOWN.

FOR SIGN BRIDGES, STRUCTURE MOUNTED, THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY AS SHOWN ON THE DRAWING. THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY TO THE BACK OF THE SIGN, BETWEEN THE ALUMINUM EXTRUSIONS, NEAR THE TOP LEFT HAND CORNER OF THE SIGN. THE BASE MATERIAL SHALL BE OMITTED AND THE FACE ADHERED DIRECTLY TO THE ALUMINUM SURFACE. PRIOR TO ADHERING THE MATERIAL, THE ALUMINUM SURFACE SHALL BE SMOOTH, CLEAN AND DRY.

WHERE SIGN BRIDGE ILLUMINATION IS PROVIDED, THE STRUCTURE MUST ALSO HAVE A SIGN BRIDGE CIRCUIT PLAQUE AS SHOWN IN THE ELECTRICAL DETAILS.



# IDENTIFICATION PLAQUE FOR SIGN BRIDGE, STRUCTURE MOUNTED

\*\* LETTER "G" UTILIZED FOR RAMP GATES. LETTER "S" UTILIZED FOR SIGN BRIDGES, OVERHEAD SIGN SUPPORTS, AND TRAFFIC SIGNALS.

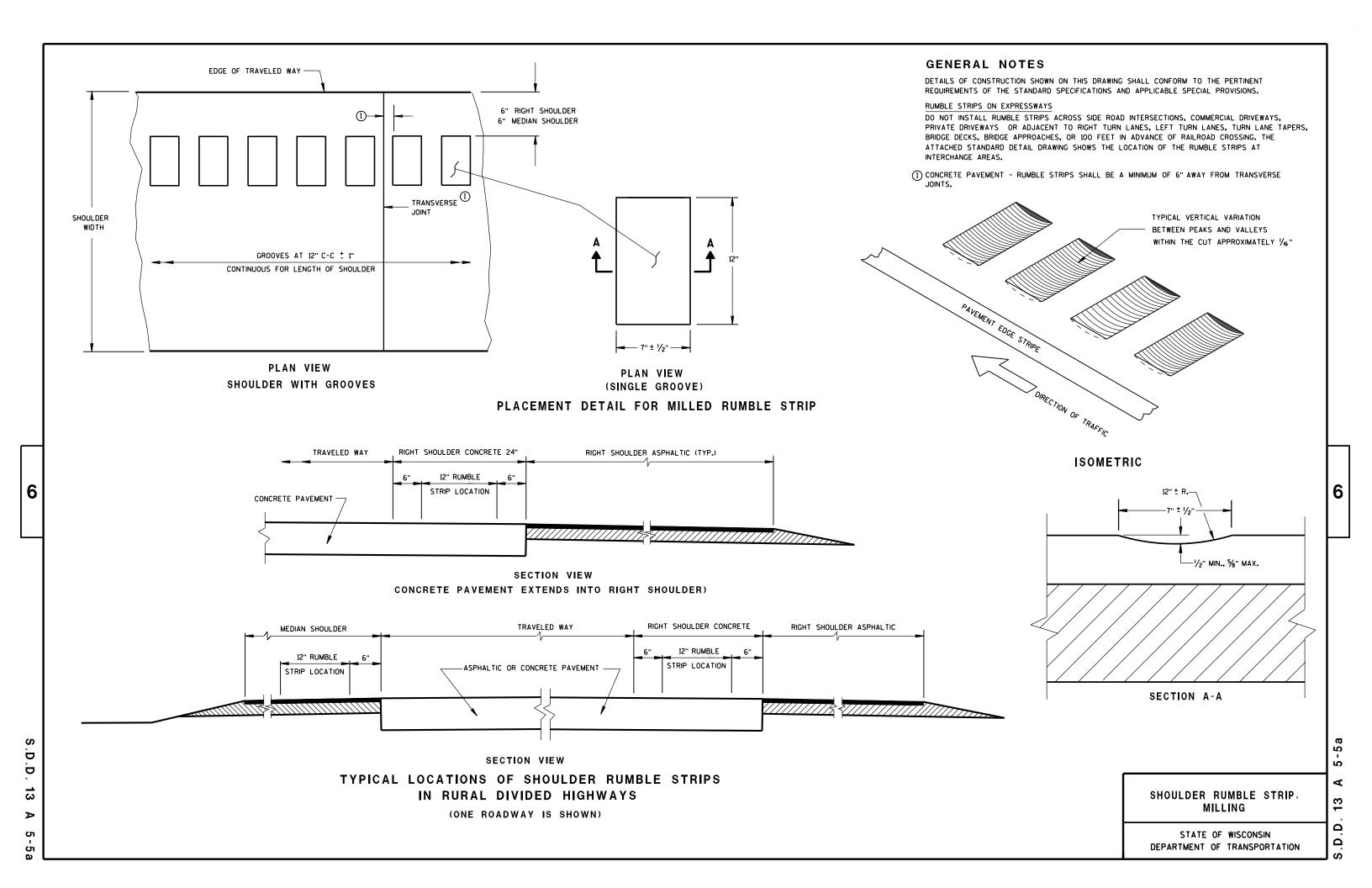
STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES, OVERHEAD SIGN SUPPORTS, & TRAFFIC SIGNALS

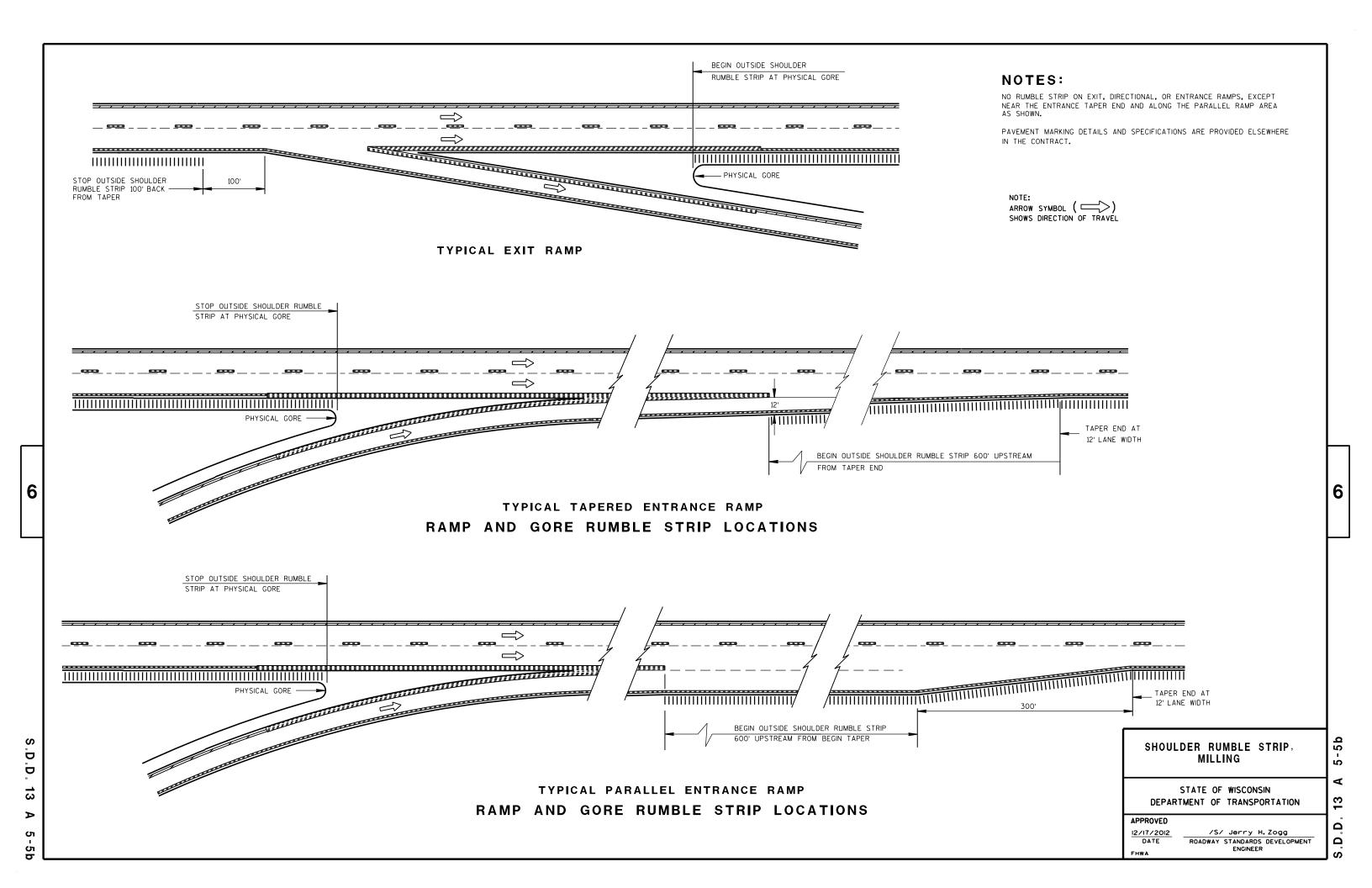
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

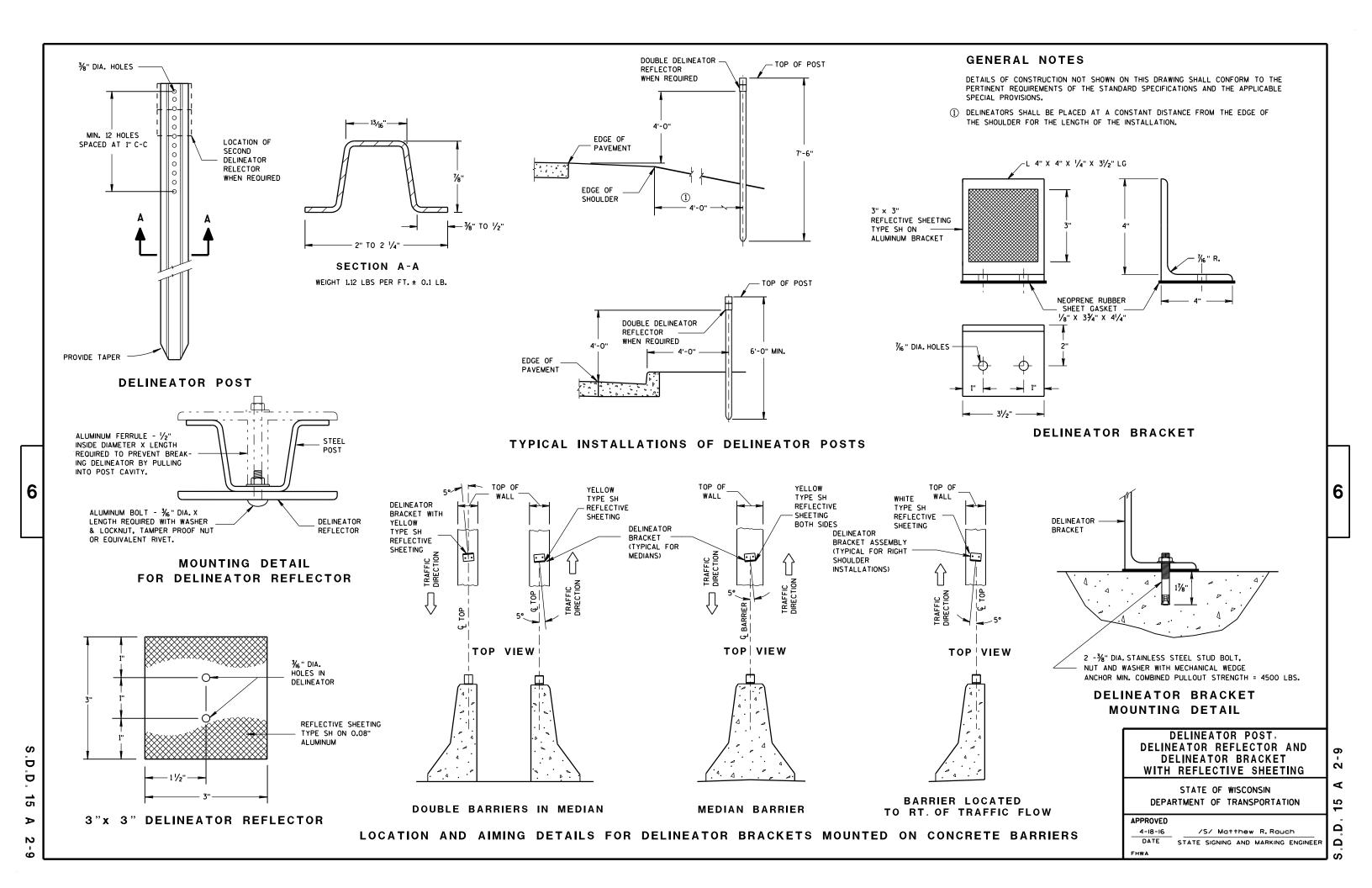
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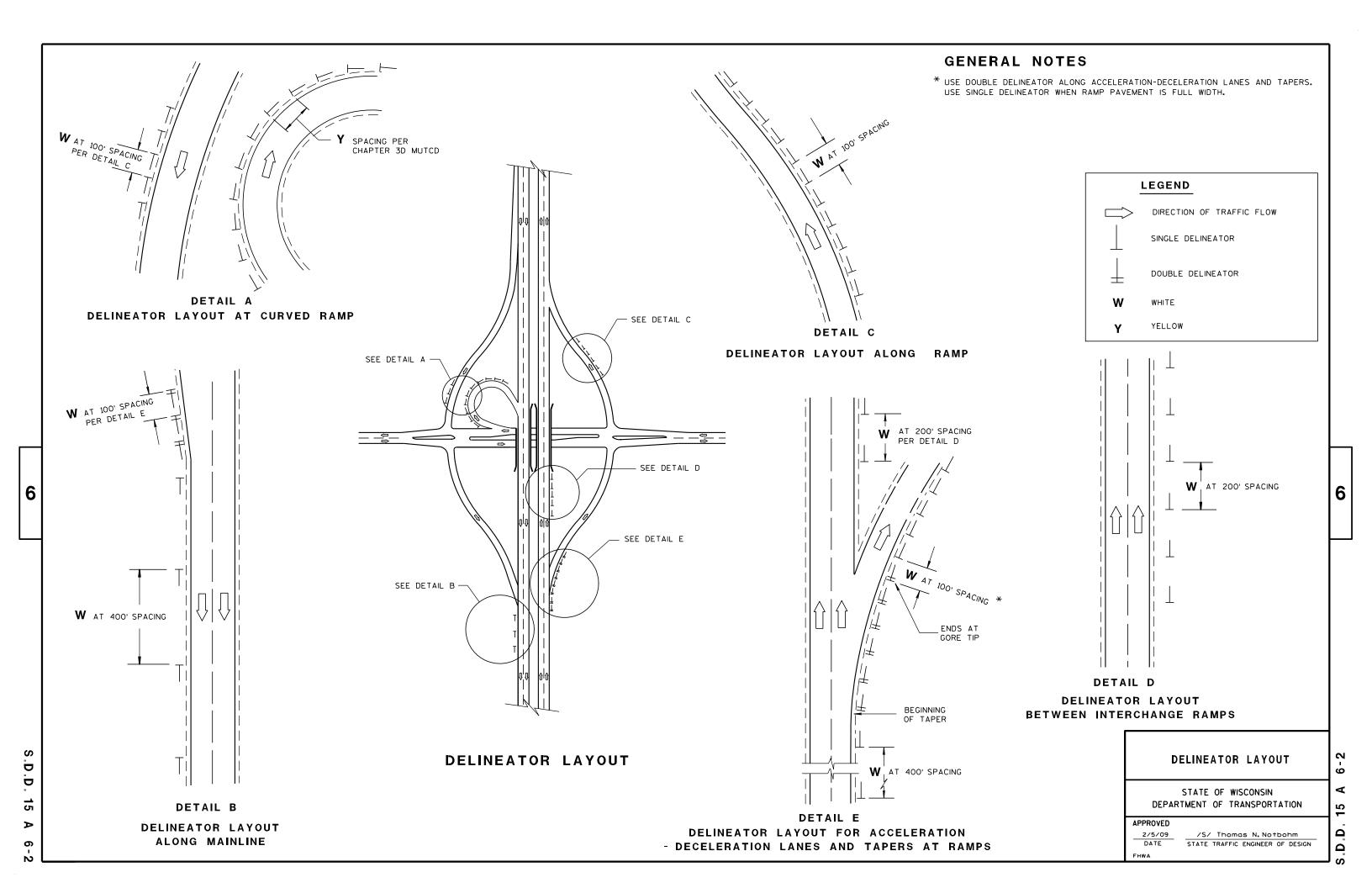
DATE STATE TRAFFIC ENGINEER OF DESIGN

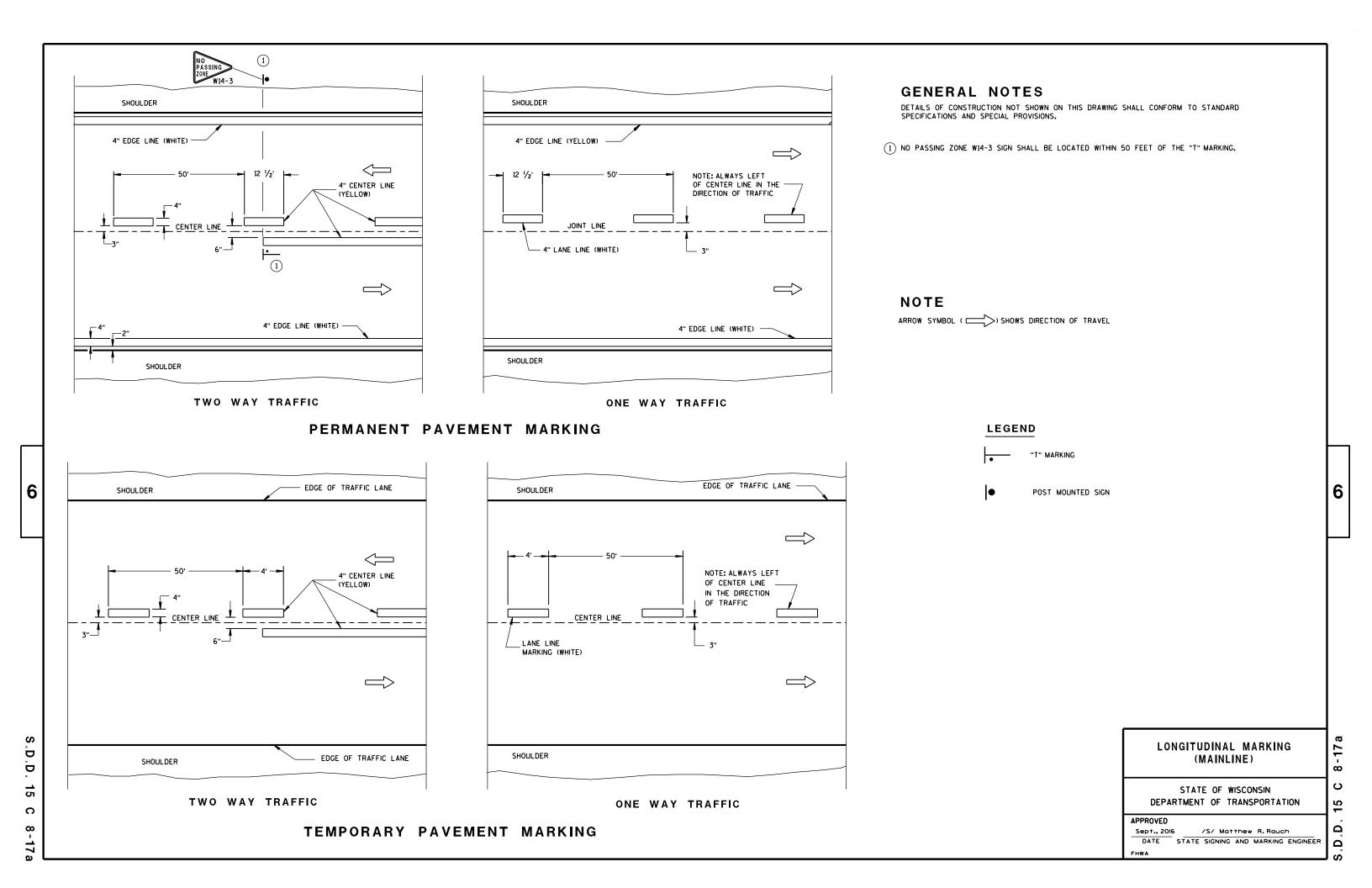
RAMP GATE, SIGN BRIDGE, OVERHEAD SIGN SUPPORT AND TRAFFIC SIGNAL STRUCTURE PLAQUE FOR SIGN BRIDGES AND OVERHEAD SIGN SUPPORT WHICH ARE NOT STRUCTURE MOUNTED

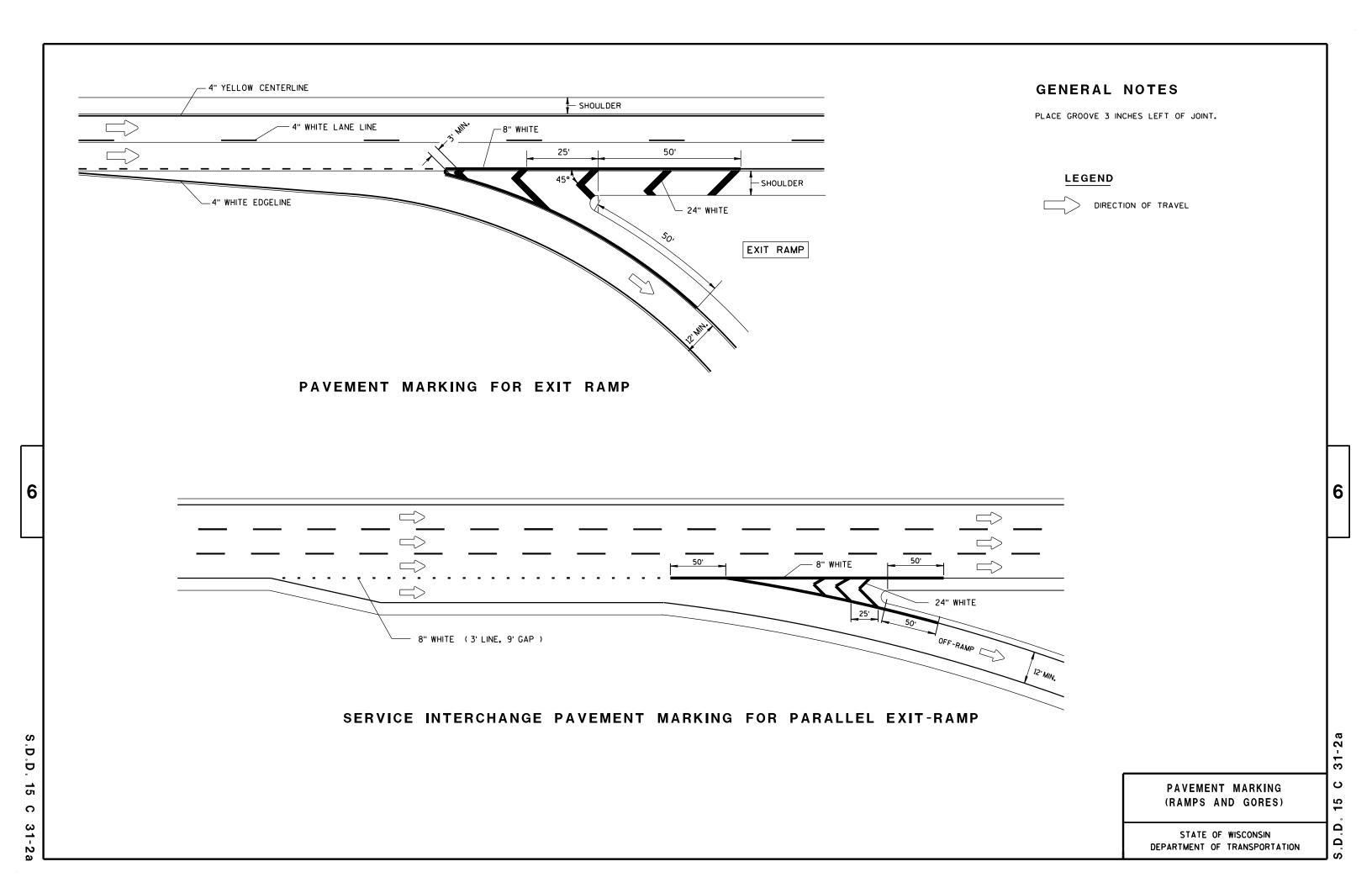


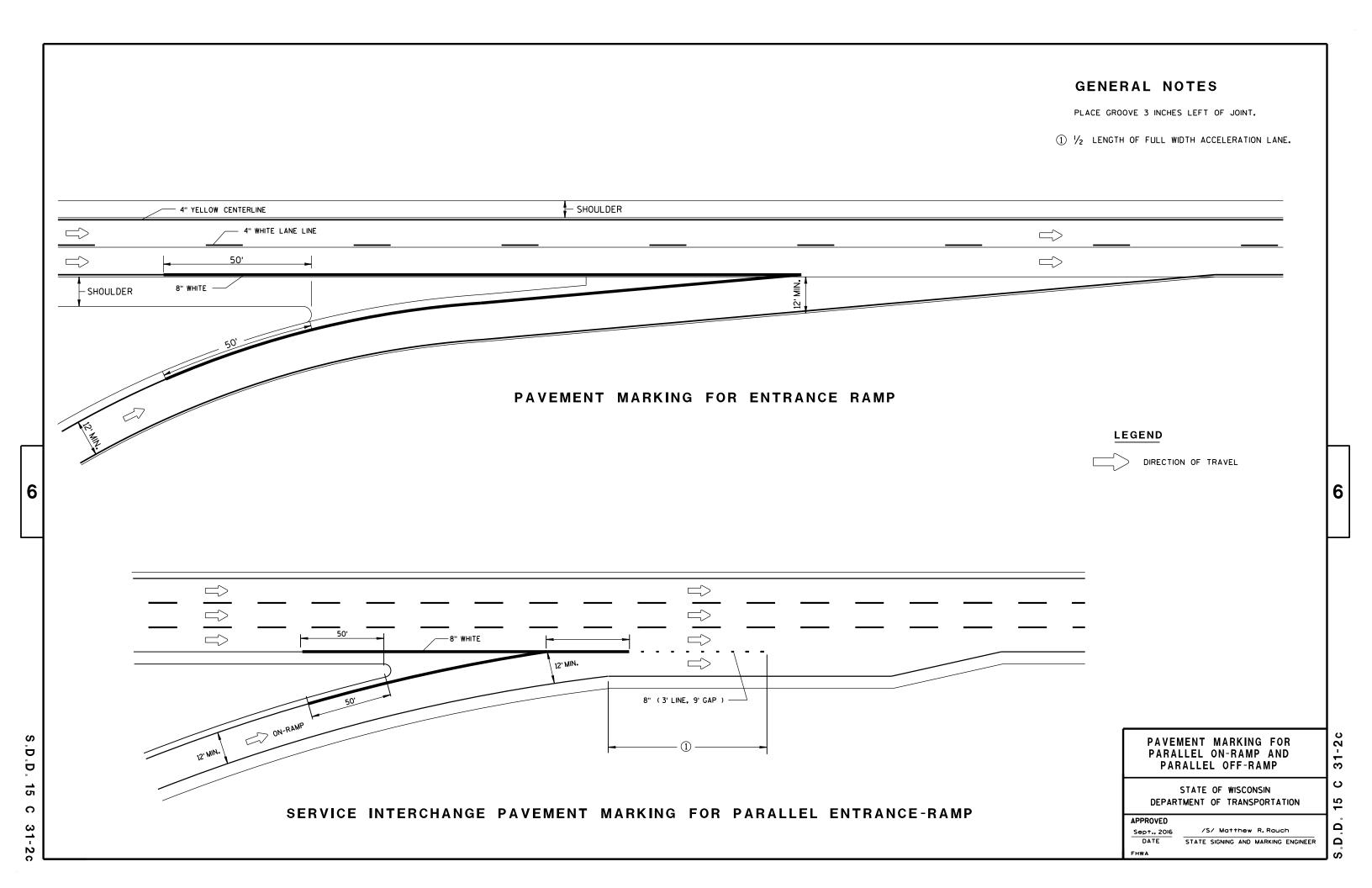




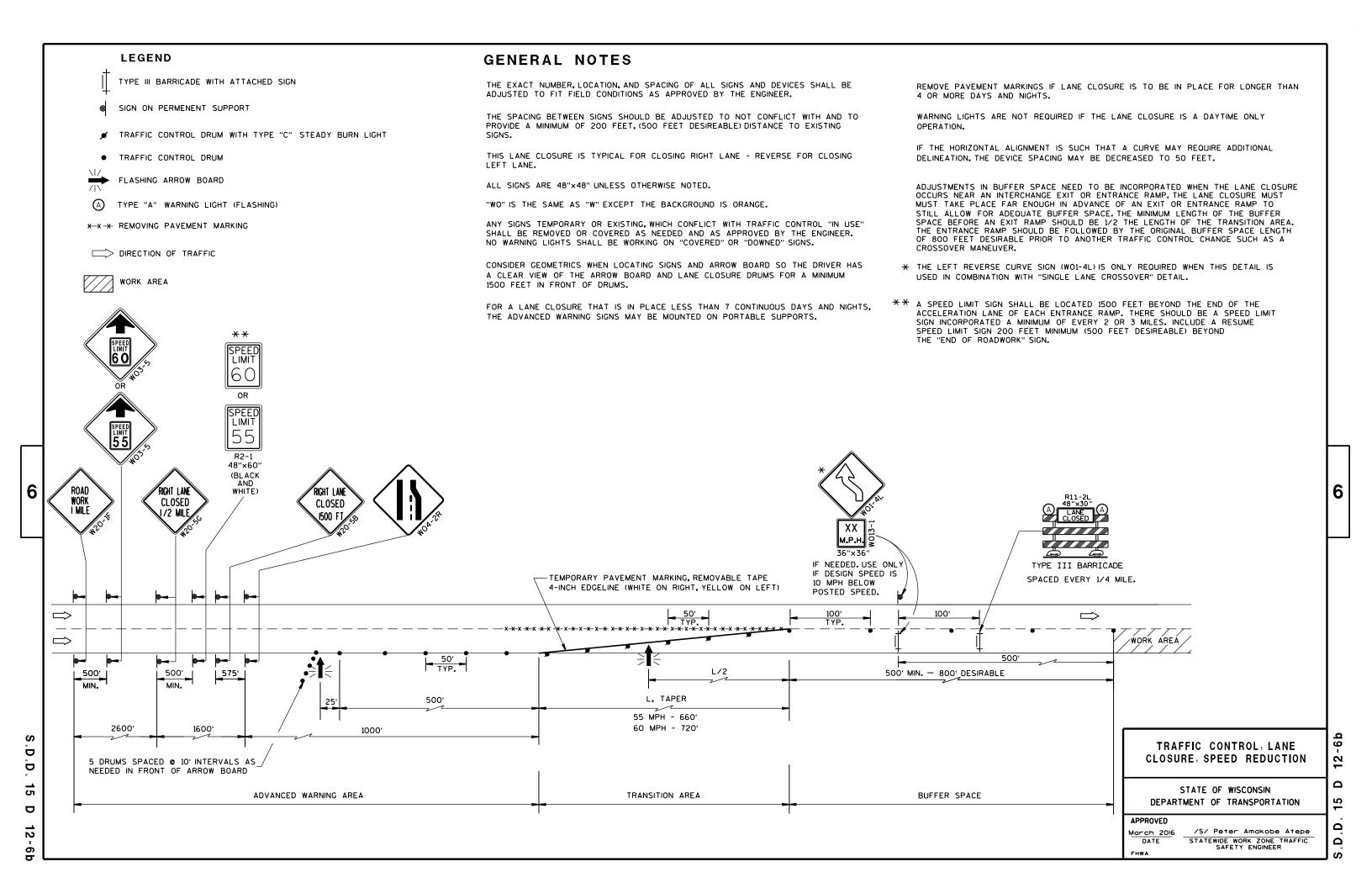


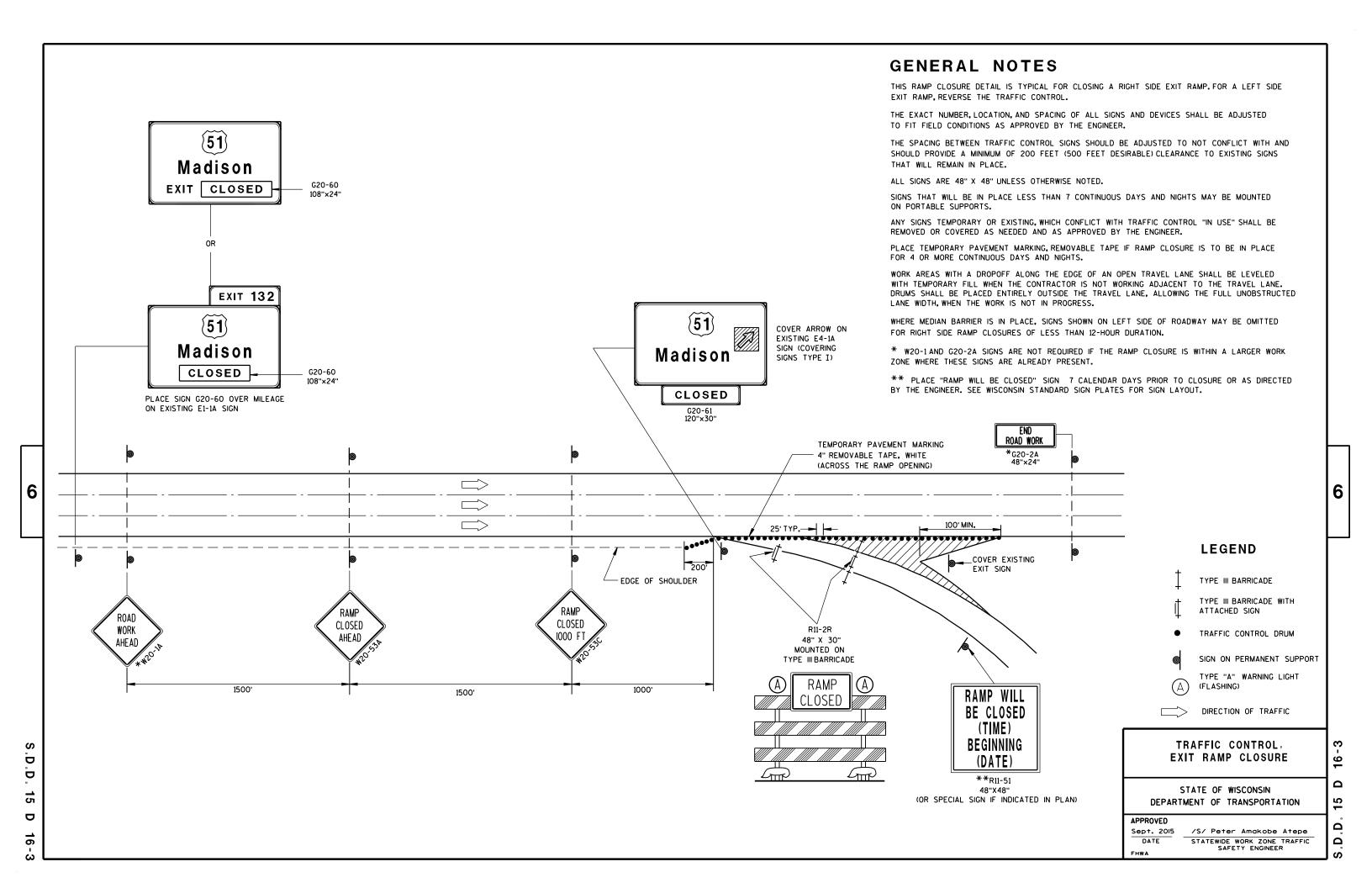


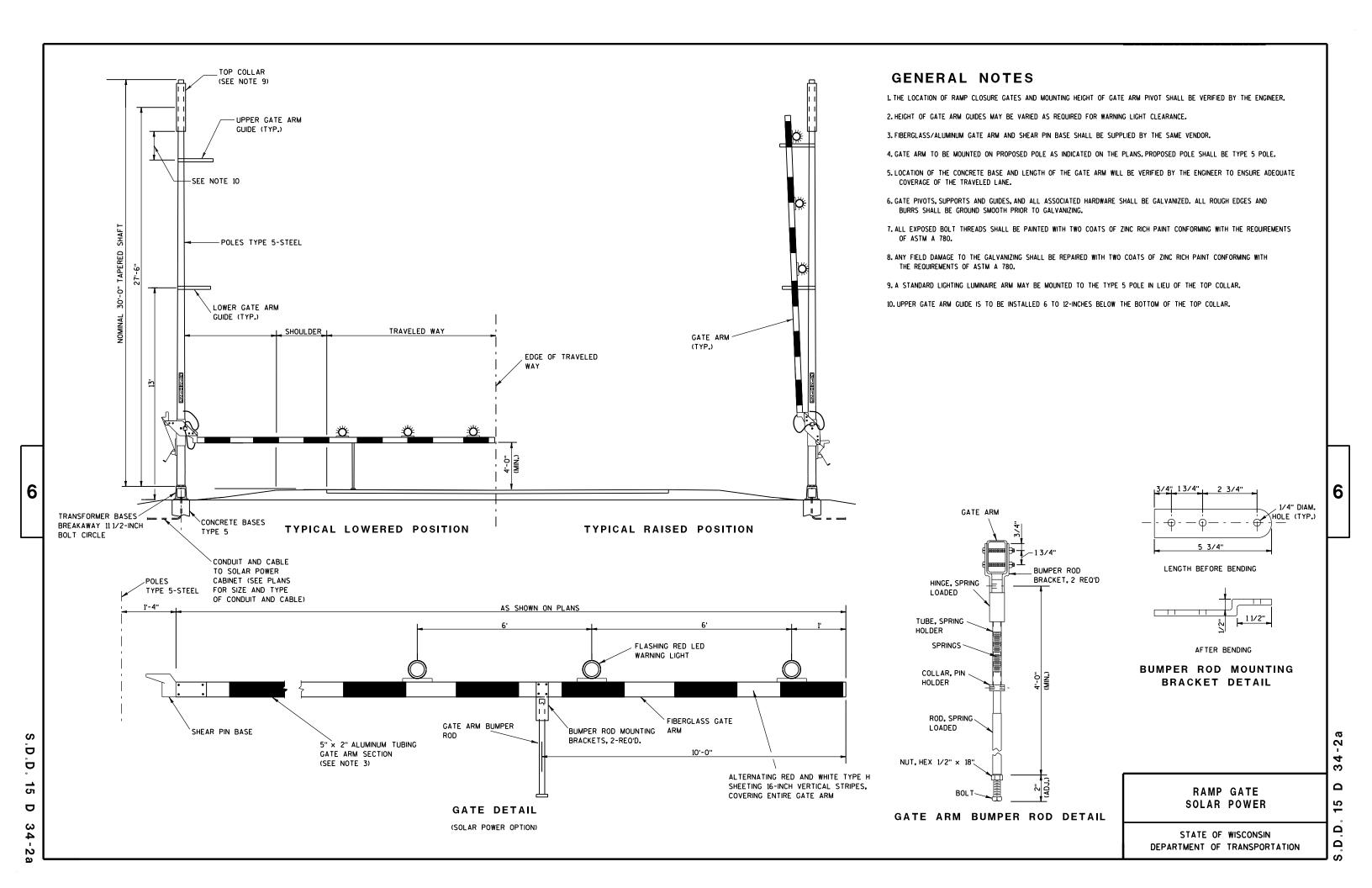


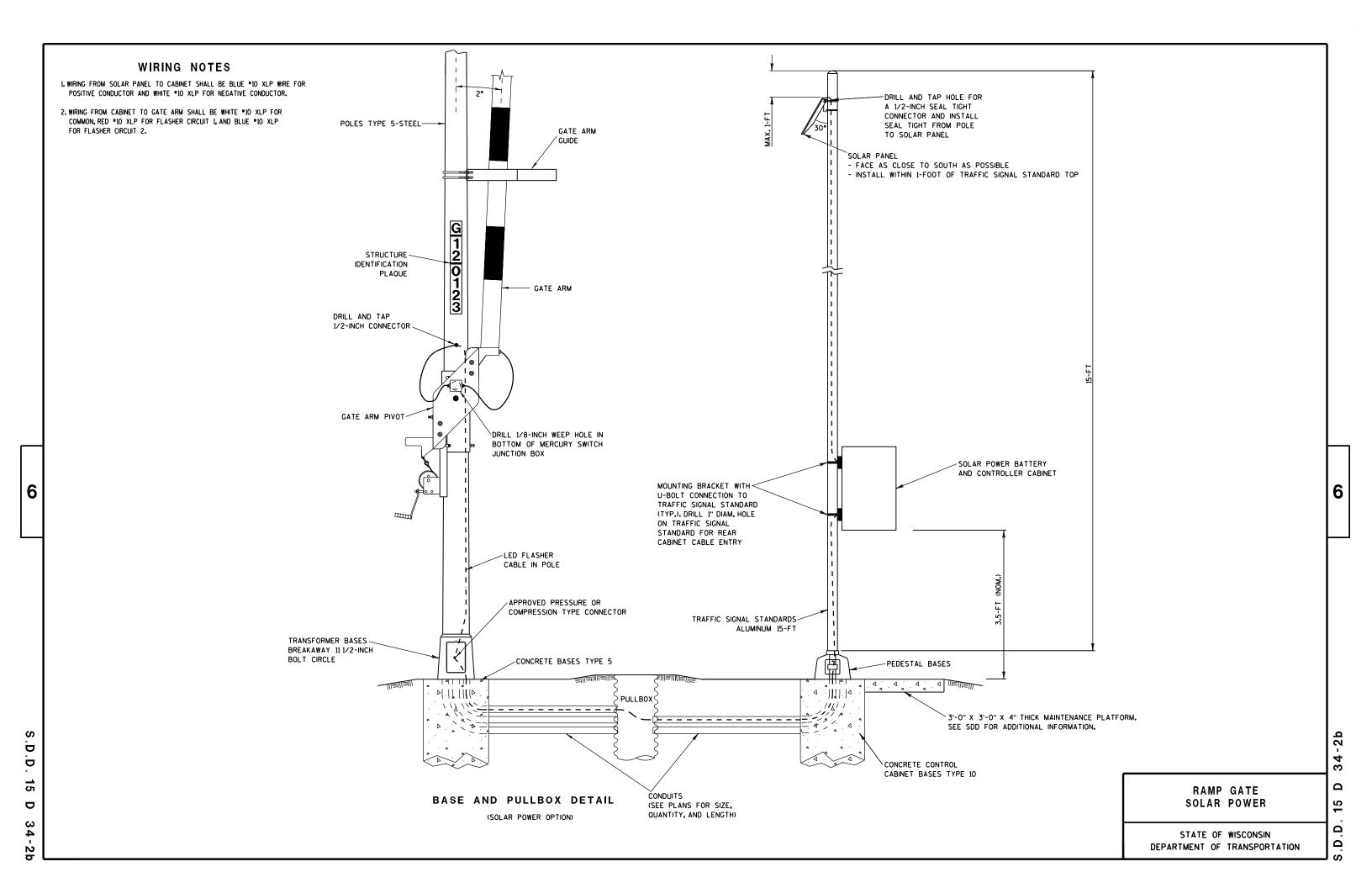


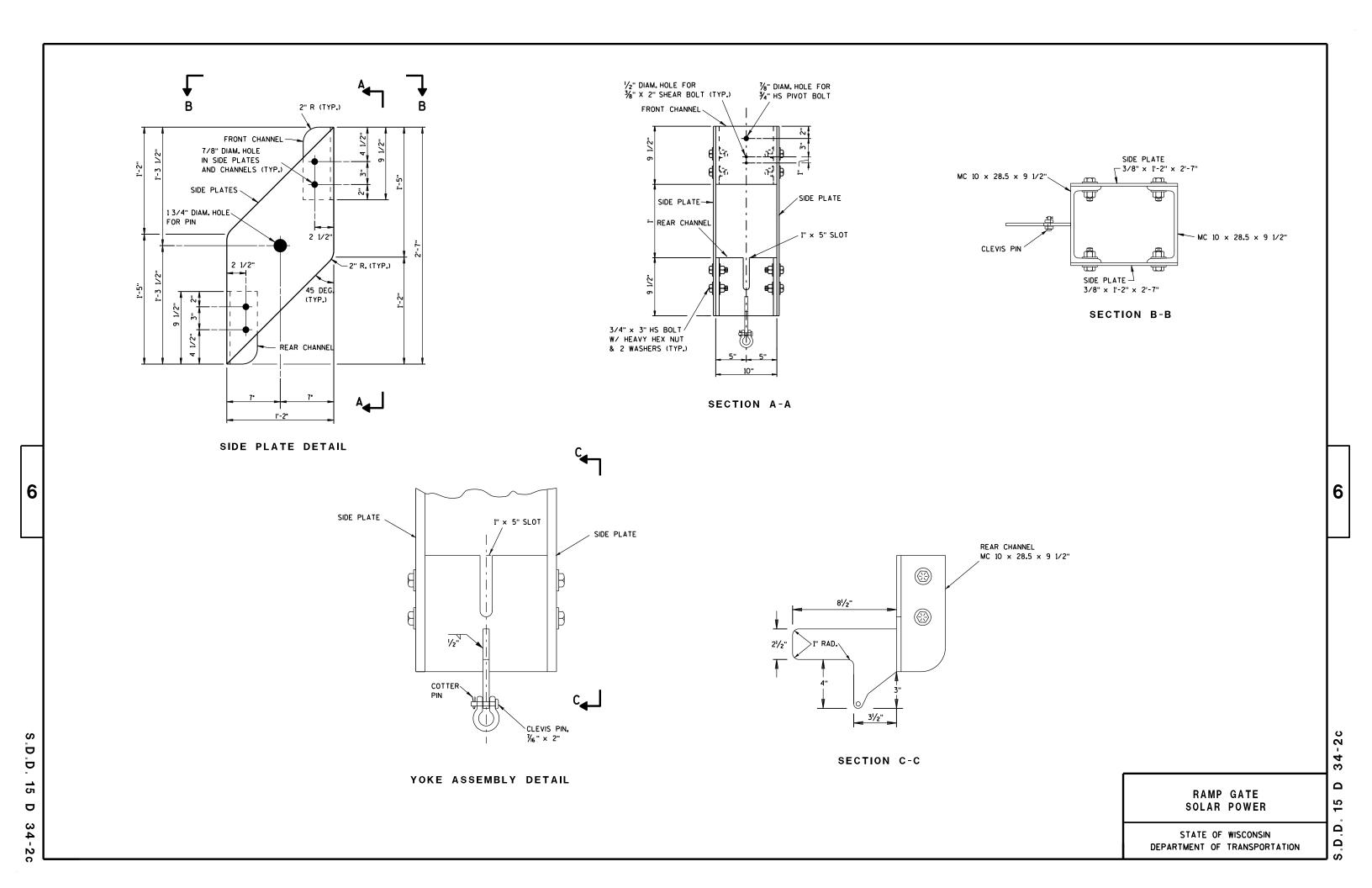
#### **GENERAL NOTES LEGEND** THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR LONGER THAN ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER. 4 OR MORE DAYS AND NIGHTS. TYPE III BARRICADE WITH ATTACHED SIGN THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION. SIGN ON PERMENENT SUPPORT IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING DELINEATION. THE DEVICE SPACING MAY BE DECREASED TO 50 FEET. LEFT LANE. TRAFFIC CONTROL DRUM ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST FLASHING ARROW BOARD "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE. MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" TYPE "A" WARNING LIGHT (FLASHING) THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS. \* X -X REMOVING PAVEMENT MARKING CROSSOVER MANEUVER. CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS \* THE LEFT REVERSE CURVE SIGN (WO1-4L) IS ONLY REQUIRED WHEN THIS DETAIL IS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM USED IN COMBINATION WITH "SINGLE LANE CROSSOVER" DETAIL. DIRECTION OF TRAFFIC 1500 FEET IN FRONT OF DRUMS. FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS. THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS. 6 6 WORK CLOSED CLOSED I MILE 1500 F XX м.Р.н 36"×36" IF NEEDED. USE ONLY TYPE III BARRICADE IF DESIGN SPEED IS TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE SPACED EVERY 1/4 MILE. 10 MPH BELOW 4-INCH EDGELINE (WHITE ON RIGHT, YELLOW ON LEFT) POSTED SPEED. 100' $\Rightarrow$ $\Rightarrow$ $\Longrightarrow$ WORK AREA 50' TYP. L/2 500' MIN. - 800' DESIRABLE 575 L. TAPER 500 50 MPH - 600' 55 MPH - 660' 2600' 1600' 1000' 60 MPH - 720' TRAFFIC CONTROL, 9 65 MPH - 780' D 70 MPH - 840' LANE CLOSURE 5 DRUMS SPACED @ 10' INTERVALS AS 2 D NEEDED IN FRONT OF ARROW BOARD 15 Δ STATE OF WISCONSIN ADVANCED WARNING AREA TRANSITION AREA BUFFER SPACE DEPARTMENT OF TRANSPORTATION D **APPROVED** /S/ Peter Amakobe Atepe 2 March 2016 STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER Ω 6 FHWA

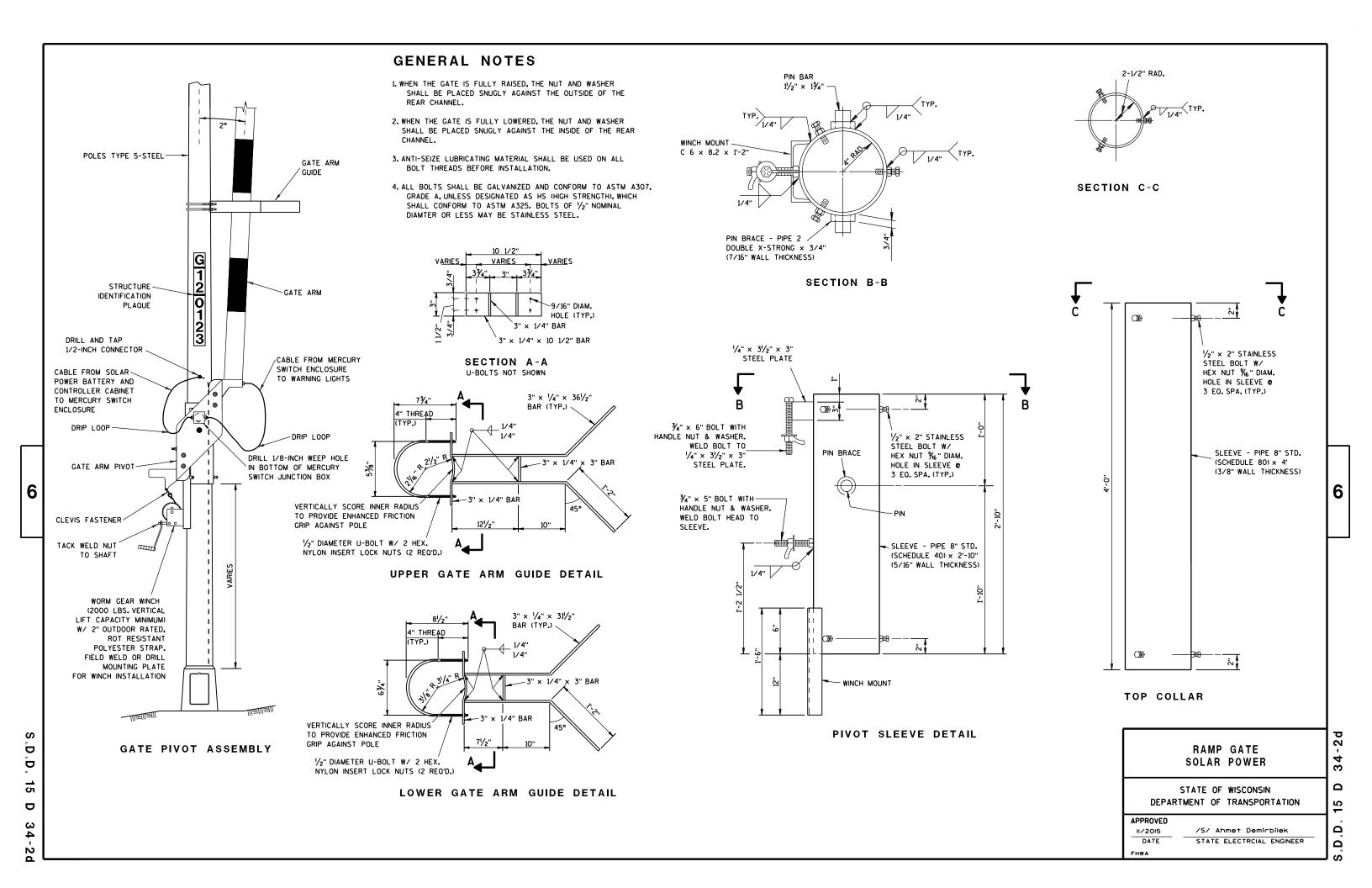


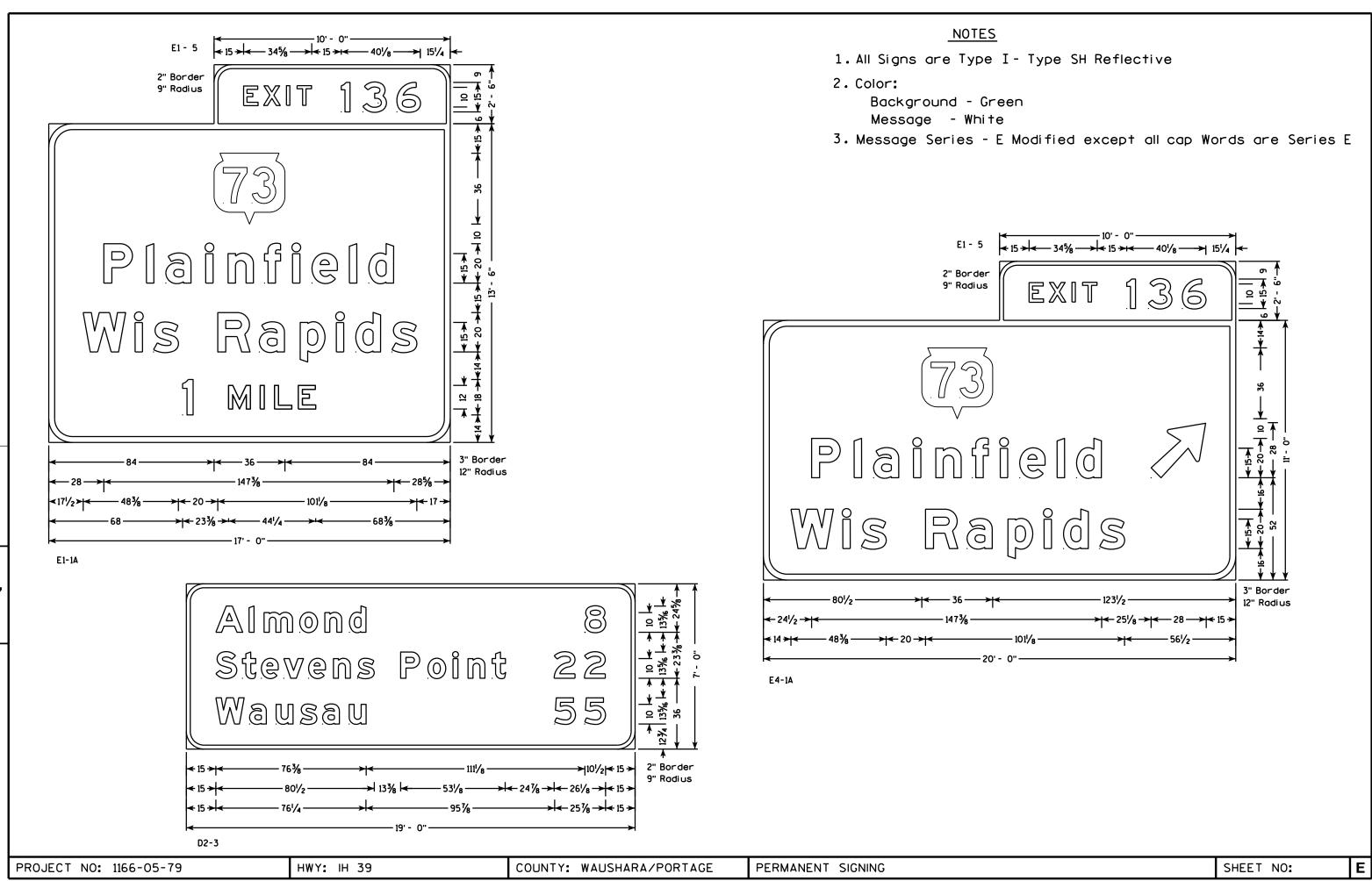










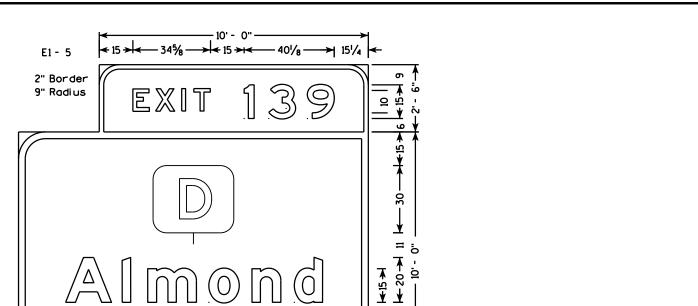


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PLOT DATE: 11-FEB-2015 09:06

PLOT NAME :

PLOT SCALE: 42.808522:1.000000



MILE

3" Border 12" Radius

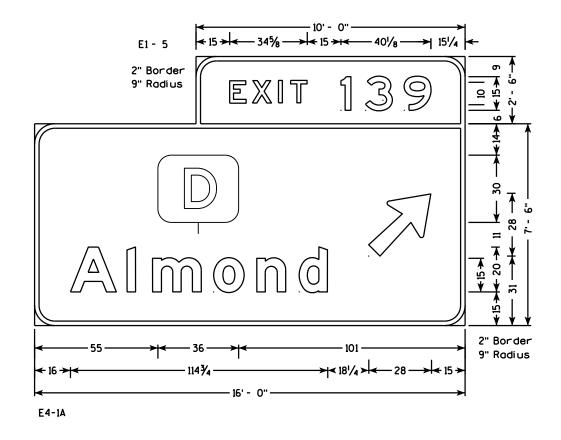
### NOTES

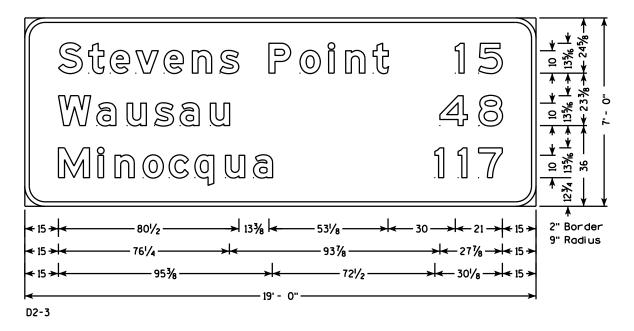
- 1. All Signs are Type I Type SH Reflective
- 2. Color:

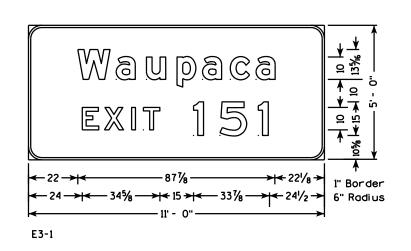
Background - Green

Message - White

3. Message Series - E Modified except all cap Words are Series E







PROJECT NO: 1166-05-79

E1-1A

HWY: IH 39

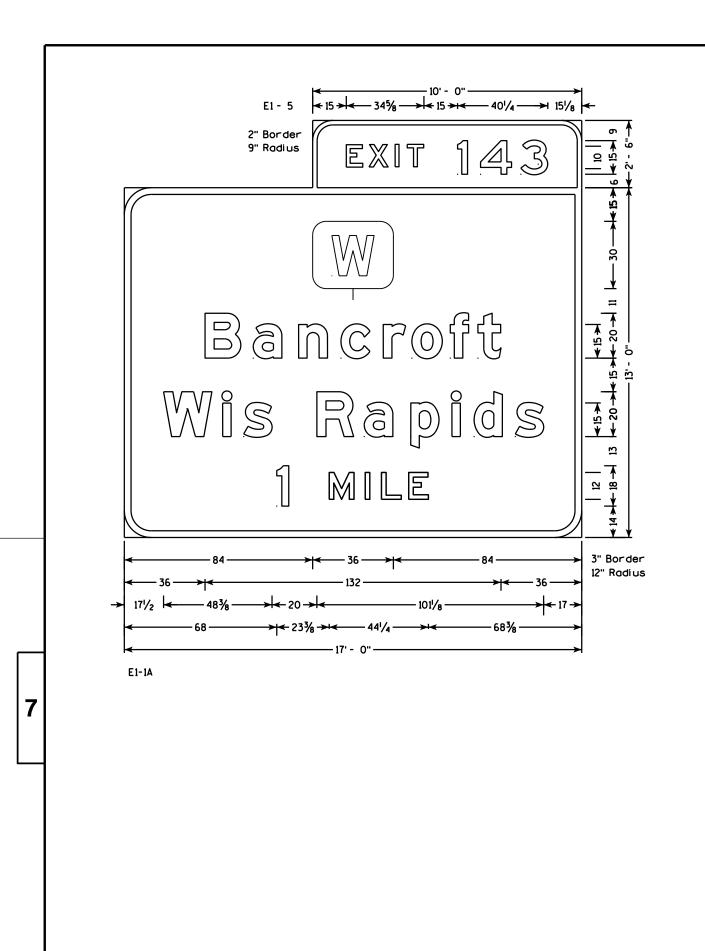
COUNTY: WAUSHARA/PORTAGE

PLOT DATE: 11-FEB-2015 09:07

PERMANENT SIGNING

PLOT NAME : PLOT

PLOT SCALE : 42.808522:1.000000



HWY: IH 39

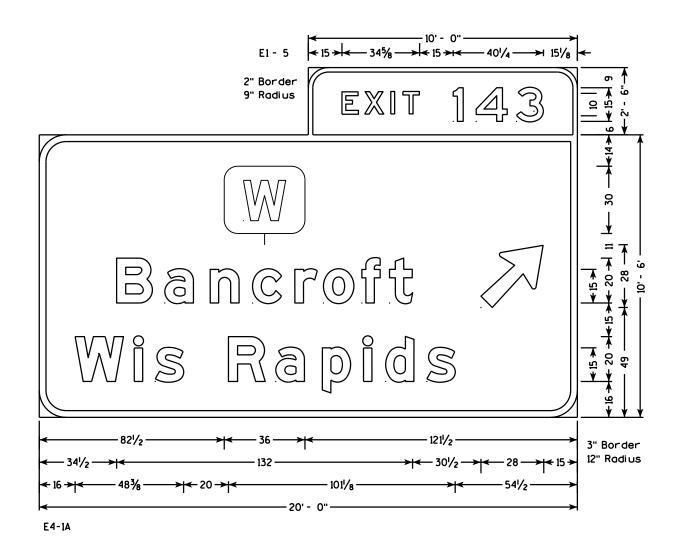
### NOTES

- 1. All Signs are Type I Type SH Reflective
- 2. Color:

Background - Green

Message - White

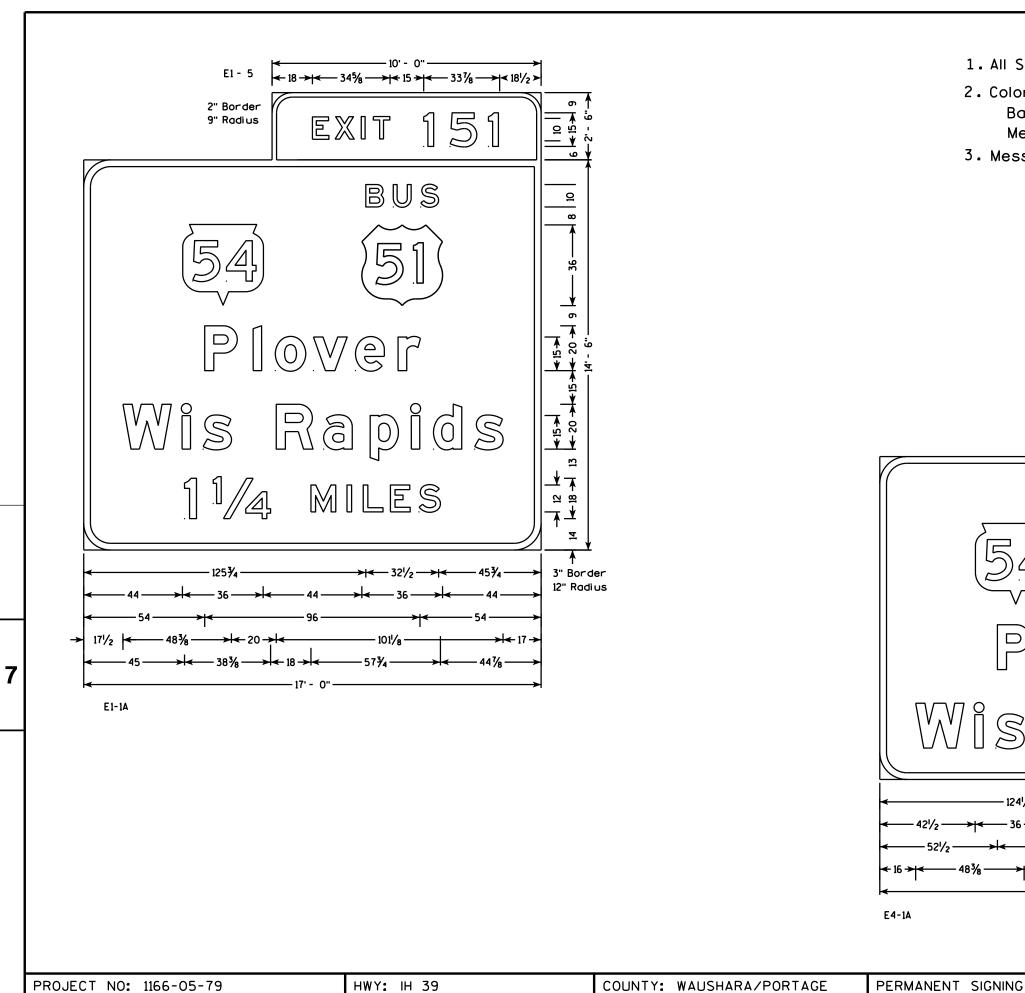
3. Message Series - E Modified except all cap Words are Series E



PROJECT NO: 1166-05-79

COUNTY: WAUSHARA/PORTAGE

PERMANENT SIGNING

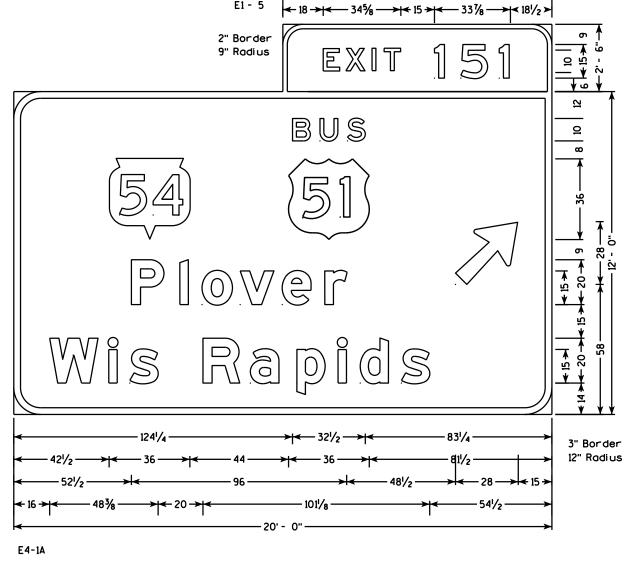


- 1. All Signs are Type I Type SH Reflective
- 2. Color:

Background - Green

Message - White

3. Message Series - E Modified except all cap Words are Series E



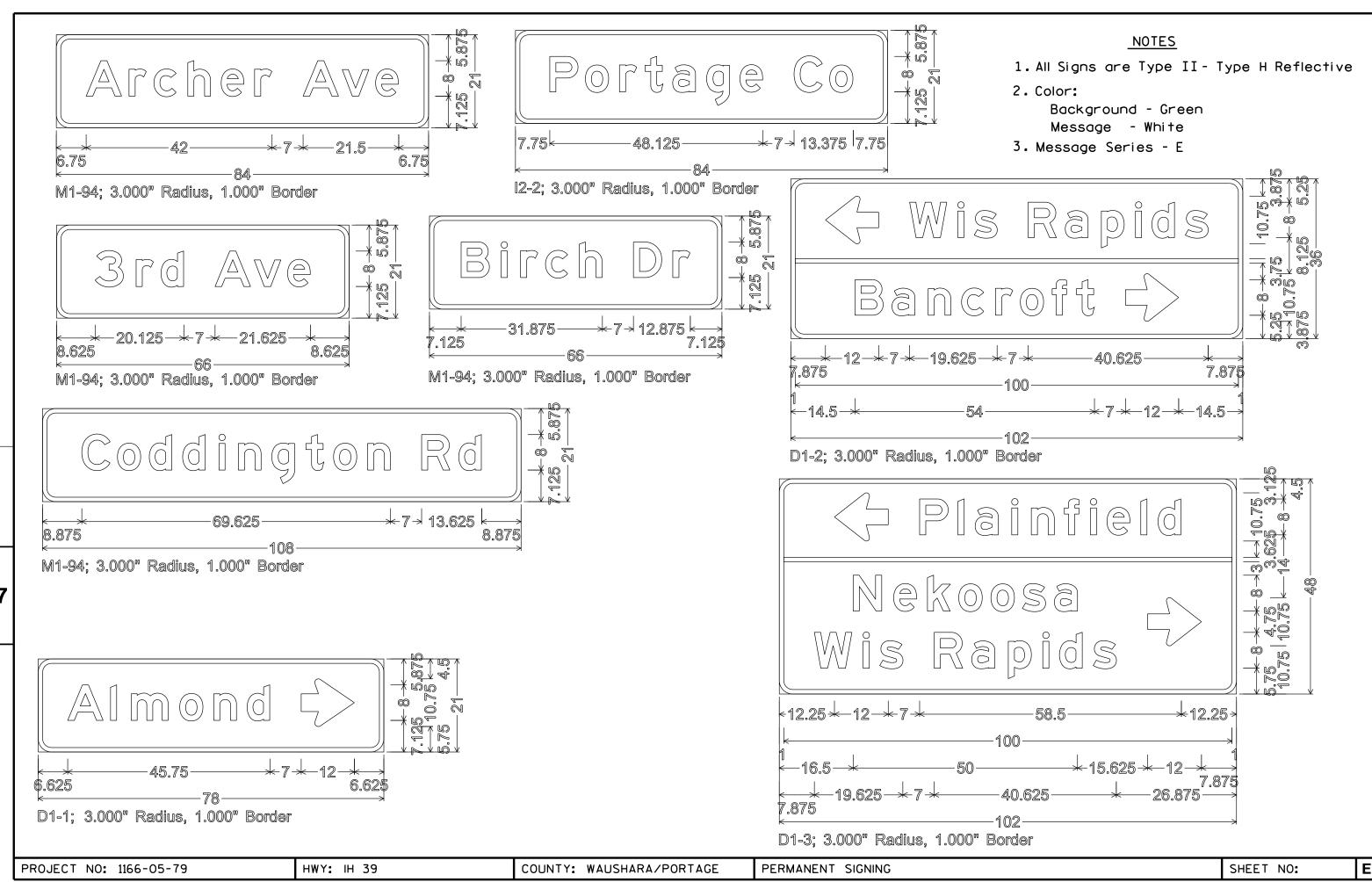
FILE NAME : C:\CAEFiles\Projects\tr\_d4\4691A215.dgn

PLOT DATE: 11-FEB-2015 09:11

PLOT NAME :

PLOT SCALE: 42.808522:1.000000

WISDOT/CADDS SHEET 42



FILE NAME : C:\CAEfiles\Projects\tr\_d4\4691A215.DGN

PLOT DATE: 11-MAR-2015 18:06

PLOT NAME

PLOT SCALE: 19.026010:1.000000

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard

areater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.



PROJECT NO:

J32-1

J22-1

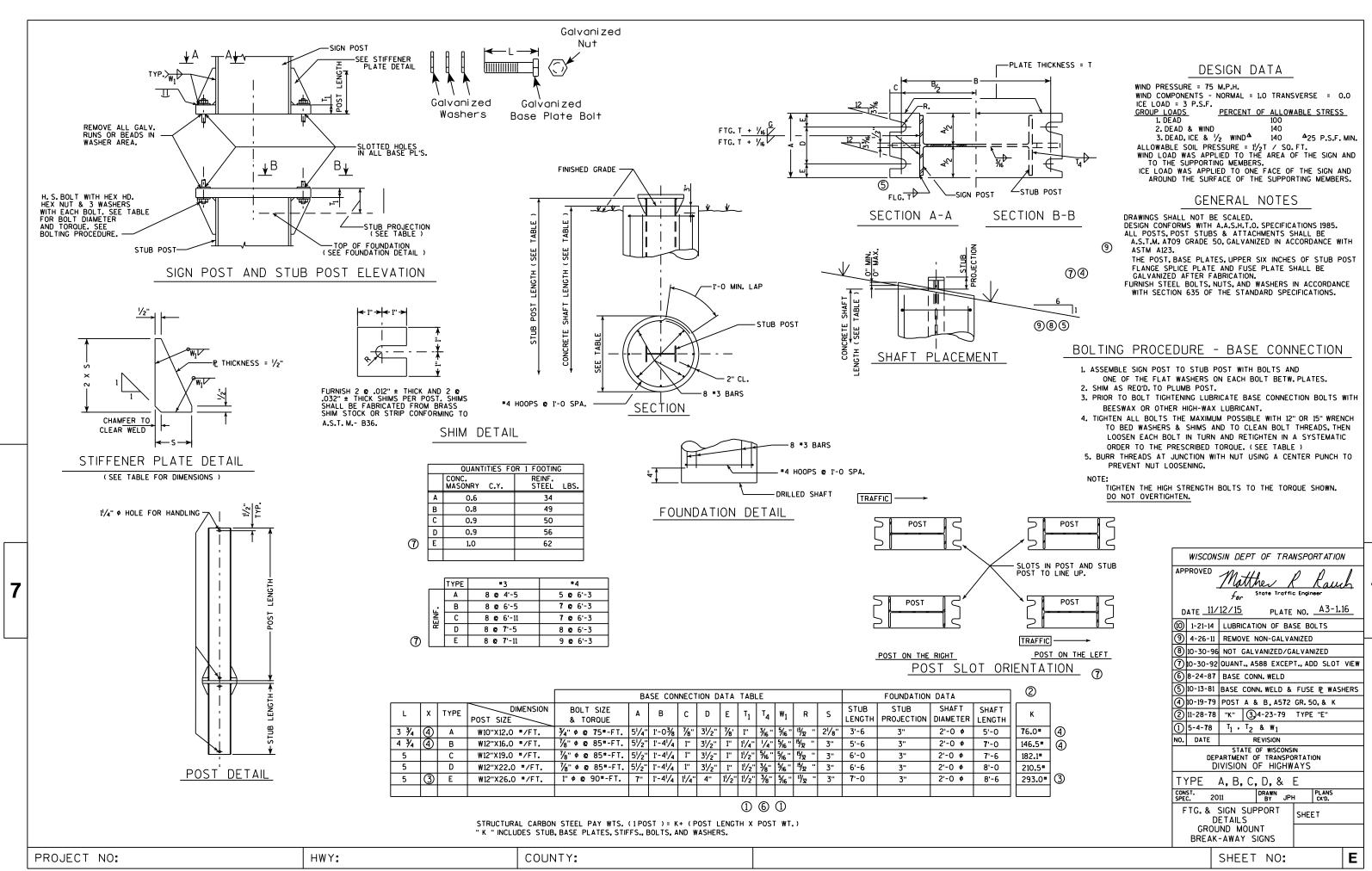
J23-1

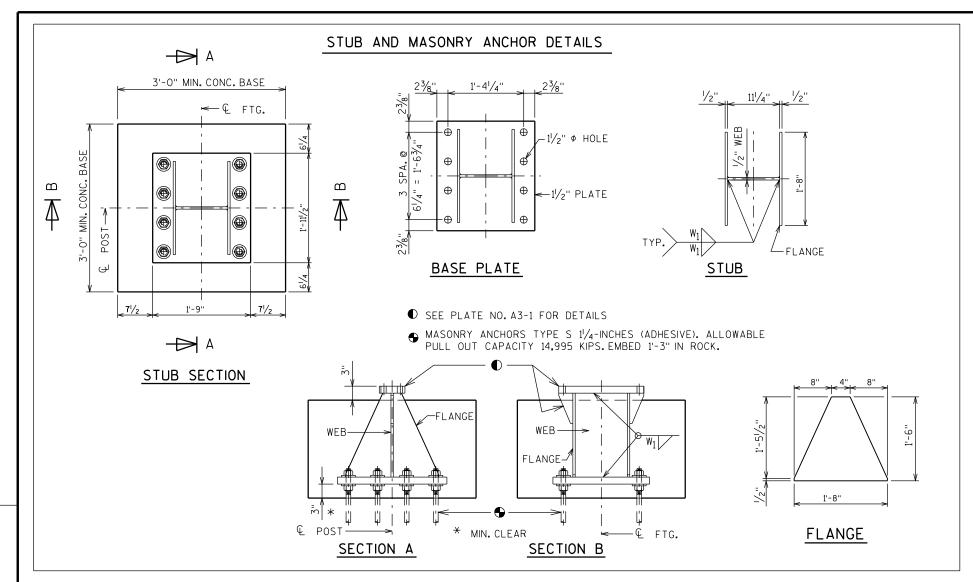
J33-1

PLOT BY: mscsja

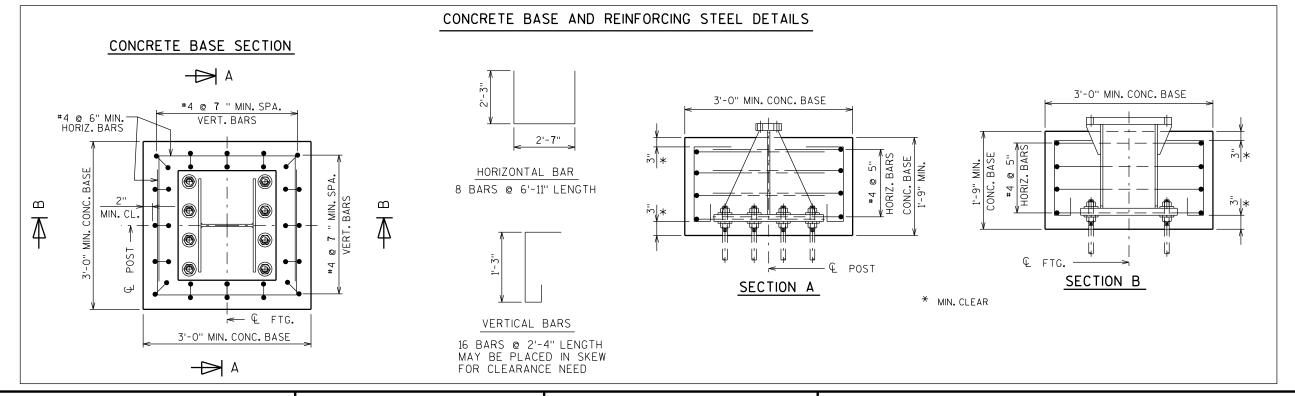
PLATE NO. \_\_A2-15.8

DATE 2/06/14





- 1. Quantities per Base:
  - REINFORCING BAR STEEL = 62 LBS
  - CONCRETE = 0.6 C.Y.
  - STEEL WEIGHT = 335 LBS
- 2. All materials, except anchor rod, nuts and washers, are to be A.S.T.M. A709 grade 50. All materials to be galvanized after fabrication.
- 3. If the contractor encounters rock before reaching the footing depth, per the A3-1 Sign Detail, determine the pull-out capacity of a test adhesive anchor installed in the rock. If the test result equals or exceeds the pull-out capacity of 14,995 KIPS, the contractor may install the breakaway stub for rock, according to this detail.



COUNTY:

ALTERNATE BREAK-AWAY
BASE ON ROCK
A3-1M

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R Rauch

For State Traffic Engineer

DATE 2/06/2014 PLATE NO. A3-1M.1

SHEET NO:

0-MAR-2014 15:16 PLOT BY: mscj9h

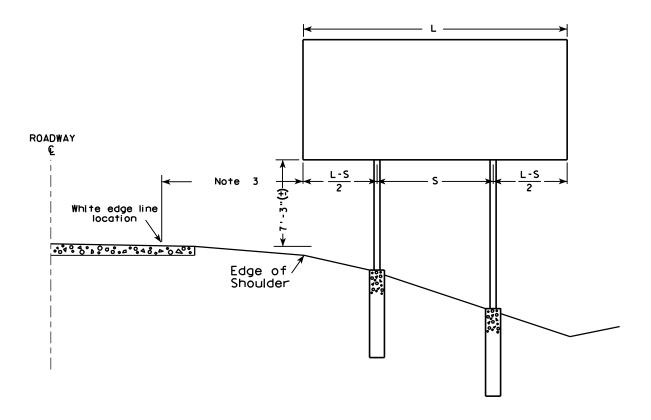
PLOT NAME :

PLOT DATE: 10-MAR-2014 15:16

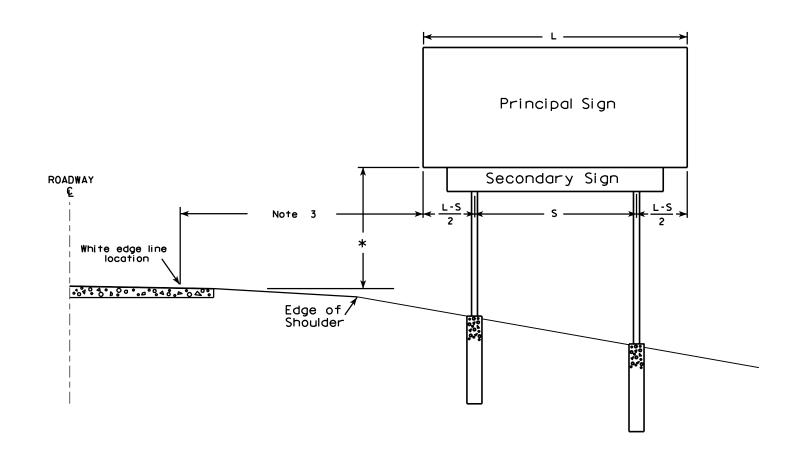
PLOT SCALE : 1.556674:1.000000

HWY:

PROJECT NO:



- 1. For a 2 post installation, S equals 3L/5, but shall not be less than 9 ft.
- 2. For a 3 post installation, S equals 5L/7, but shall not be less than 18 ft., and the space between any two posts shall not be less than 9 ft.
- 3. Unless noted in the plan, the sign offset distance shall be a minimum of 17'-6", desirable 30'-0".
- 4. The (+) tolerance shown on this sheet is 3 in.
- 5. The vertical sign height clearance detailed is measured from the bottom of the sign to the near edge of pavement.
- 6. Post lengths shown in the miscellaneous quantities are estimated lengths. The contractor shall verify post lengths at the time of final grading.
- 7. Refer to the Traffic Guidelines Manual for further guidance on minimum vertical clearance requirements.



\* Clearance is  $8'-3''(\pm)$  when the secondary sign is 3 ft. or less in height. For secondary signs larger than 3 ft., the clearance to the bottom of the secondary sign shall be  $5'-3''(\pm)$ .

> TYPICAL INSTALLATION OF TYPE I SIGNS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED for State Traffic Engineer PLATE NO. <u>A4-1.9</u>

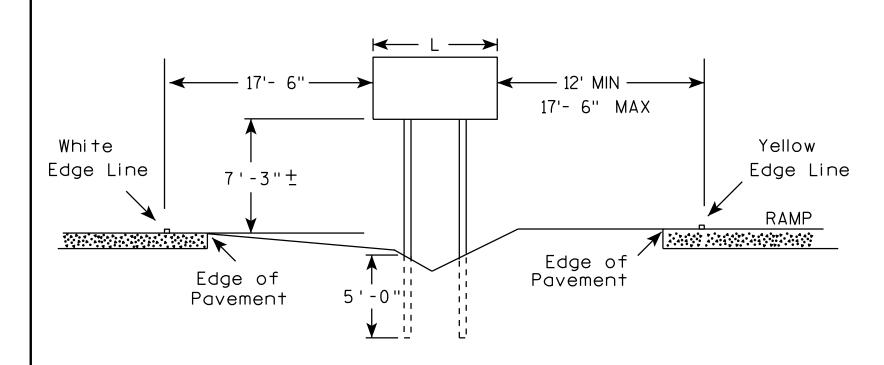
DATE 4/02/08

SHEET NO:

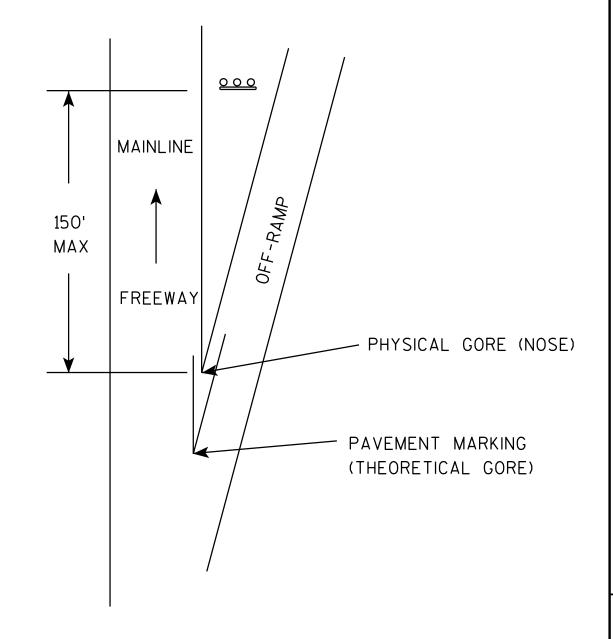
PROJECT NO:

PLOT DATE: 02-APR-2008 15:49

PLOT BY : ditjph



- 1. The 150 foot distance from the physical gore (where pavement ends) will normally provide the offsets as shown.
- 2. If roadway geometrics permit, the sign may be closer than the 150 foot distance as long as the offsets are maintained.
- 3. At no time shall the location be greater than 150 feet. If the normal offsets cannot be maintained, they can be reduced to 6 feet from the edge of the paved shoulder (both freeway and ramp).
- 4. The offset from edge of sign to the yellow edge line on the ramp is shown as a minimum of 12 feet and a maximum of 17 feet, 6 inches. Preference is adhering to the maximum rather than the minimum dimension.
- 5. When L is equal to or exceeds 10 feet, use 3 posts as per A4-4.
- 6. The  $(\overline{+})$  tolerance for the mounting height is 3 inches.



TYPICAL INSTALLATION OF TYPE II SIGNS ON WOOD POSTS IN GORE

WISCONSIN DEPT OF TRANSPORTATION

DATE 2/06/14

PLATE NO. 44-2.3

SHEET NO:

PROJECT NO:

PLOT DATE: 06-FEB-2014 12:36

PLOT BY: mscsja

## URBAN ARFA



RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



5'-3"(生)  $D^{-1}$ Outside Edae of Gravel

White Edgeline Location

\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where

there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

PLOT BY : mscj9h

### GENERAL NOTES

- 1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is  $7'-3''(\pm)$  or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A2-1S) is  $7'-3''(\pm)$  or  $6'-3''(\pm)$ per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is  $5' - 3'' (\pm)$ .
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (+) tolerance for mounting height is 3 inches.
- 8. Folding signs shall be mounted at a height of 5'-3'' ( $\pm$ ) or as directd by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3"  $(\pm)$ . The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3'' ( $\pm$ ).

### POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
( Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

APPROVED

for State Traffic Engineer

DATE 7/23/15

PLATE NO. <u>A4-3.20</u>

FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\A43.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:21

COUNTY:

PLOT NAME :

PLOT SCALE: 99.237937:1.000000



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



### ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



### PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3'' (±) or 6'-3'' (±) per urban or rural detail respectively.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).
- \* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- \*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- \*\*\* See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

### URBAN AREA RURAL AREA (See Note 3) 2'Min - 4'Max (See Note 6) ₩E# FF# 6'-3"(±) 6'-3"(±) 7'-3"(±) \*\* Curb **\*\*\*\***\ Flowline D **7000** White Edgeline D 11 White Edgeline, Location Outside Edae Location

# 2' Min - 4' Max (See Note 6) 6'-3"(±) Curb Flowline. -11

48" DIAMOND WARNING SIGN

HWY:

# \_ 26" 5 ' - 3 "(±) White Edgeline Location Outside Edge of Gravel 48" DIAMOND WARNING SIGN

COUNTY:

Outside Edge

of Gravel

	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRED		
	L	E	
<del>* * *</del>	Greater than 48" Less than 60"	12"	
	60" to 120"	L/5	l

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN (FOUR POSTS REQUIRE	
L	E
168" and greater	12"

### POST EMBEDMENT DEPTH

of Gravel

Area of Sign	
Installation	D
( Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

Matther

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr\_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:23

PLOT SCALE: 107.021305:1.000000

WISDOT/CADDS SHEET 42

PLOT NAME :

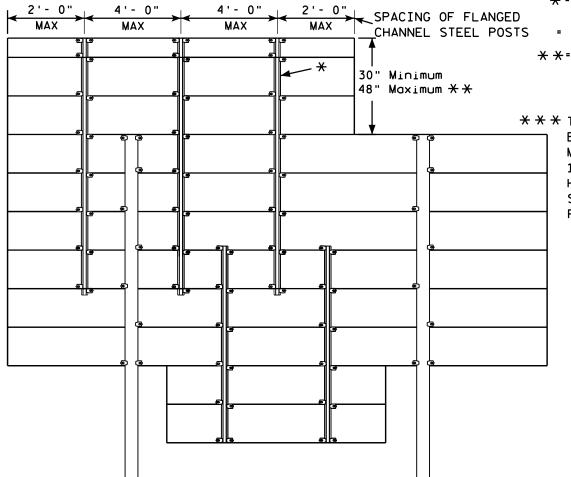
PLOT BY: mscj9h

WISCONSIN DEPT OF TRANSPORTATION APPROVED

For State Traffic Engineer

PLATE NO. 44-4.14 DATE 7/23/15





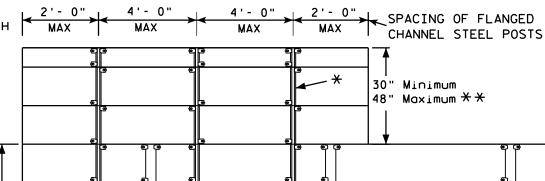
\*=2.00 lb/ft FLANGED CHANNEL, MIN. YIELD STRENGTH

CHANNEL STEEL POSTS = 60,000 PSI (GRADE 60) GALVANIZED

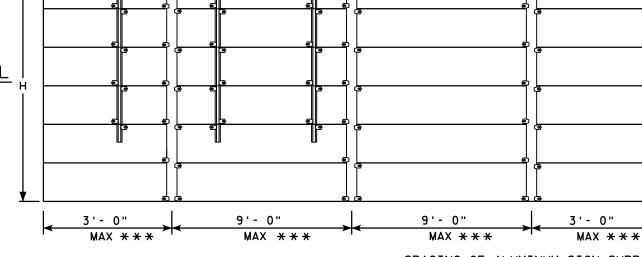
SIGN BRIDGE MOUNTED SIGN

\* \*= FOR 48" HEIGHT PANELS ON OVERHEAD STRUCTURES, ENTIRE SIGN SHALL BE CENTERED VERTICALLY ABOUT THE DEPTH OF THE TRUSS.

\* \* THESE SPACING DISTANCES SHALL ONLY BE USED WHEN THE MAIN SIGN HAS A MAXIMUM HEIGHT (DIMENSION H) OF 16 FT OR LESS. FOR SIGNS WITH A HEIGHT OF GREATER THAN 16 FT, STRUCTURAL CALCULATIONS SHALL BE PERFORMED.



FLANGE CHANNEL DETAIL 1/<sub>4</sub> → NOT TO SCALE



SPACING OF ALUMINUM SIGN SUPPORTS 5" X 3.5" X 3.7 LBS./ft.

### GENERAL NOTES

- 1. Flanged channel steel posts shall conform to size and material above, and shall be considered as incidental to other items in the contract.
- 2. Number of Flanged channel steel supports varies with length of panel and shall be spaced as shown:

PANEL LENGTH 8'-0" OR LESS = 2 CHANNELS PANEL LENGTH 9'- 0" - 12'- 0" = 3 CHANNELS PANEL LENGTH 13'- 0" OR MORE = 4 CHANNELS

If the flanged channel steel posts can not be horizontally spaced as shown, they can be moved so as to securely hold the sign.

3. The EXIT NUMBER PANEL shall normally be positioned above the guide sign aligned with the right edge of the guide sign. If the guide sign indicates a left exit, the EXIT NUMBER PANEL shall be aligned with the left edge of the guide sign.

2'- 0"

- 4. If the bolt holes in the top panel (EXIT NUMBER), or sub panel (NEXT EXIT) line up with holes in main sign panel, stitch bolts shall be used in addition to the channels.
- 5. Provide post clips for each sign as shown. (Please note the differences between a ground mounted versus Sign bridge mounted sign as far as number of clips required on the main supports or beams)
- 6. Structural steel sign supports shall extend to the top of the main signs, as shown on the above details.

PLOT BY: mscs.ja

ATTACHMENT OF GUIDE SIGNS TO SUPPORTS

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 12/05/13

PLATE NO. A4-6.12

SHEET NO:

PROJECT NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\A46.DGN



Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Nather R Raw
For State Traffic Engineer

DATE <u>8/11/16</u>

PLATE NO. <u>44-8.8</u>

PROJECT NO:

FILE NAME : C:\CAFfiles\Projects\tr stdplote\A48 DCN

PLOT DATE . 11-416-2016 11:35

PINT RY \* \$\$ nintuser \$\$

SHEET NO:

| | |



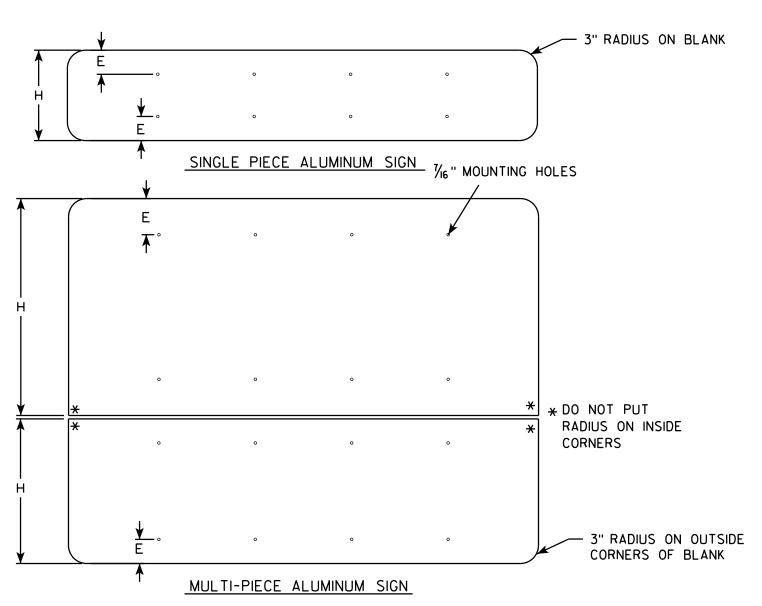
PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

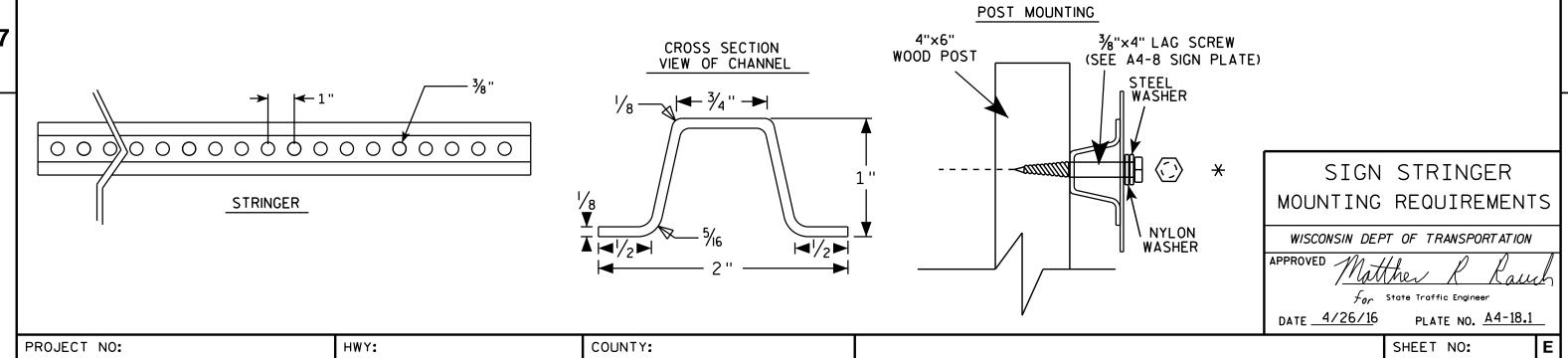
For State Traffic Engineer



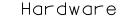


- ALL SIGNS OVER 60" IN WIDTH SHALL HAVE A 3" RADIUS ON THE OUTSIDE CORNERS OF THE ALUMINUM BLANK.
- MOUNTING HOLES SHALL BE  $\frac{7}{16}$ " DIAMETER.
- SEE CHART FOR HOLE SPACING REQUIREMENTS
- FOR SIGN PANELS WITH DIMENSION (H) 36" AND OVER, DIMENSION E SHALL BE 6"
- FOR SIGN PANELS WITH DIMENSION (H) UNDER 36", DIMENSION E SHALL BE 4"
- SIGN STRINGER MATERIAL SHALL CONSIST OF STEEL CHANNEL POST SECTIONS, WEIGHING 1.12 LBS/FT IN ACCORDANCE WITH SECTION 633.2.1 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION.
- SEE SIGN PLATE A4-8 FOR SIGN STRINGER BOLTING REQUIREMENTS.

SIGN WIDTH	STRINGER WIDTH	POSTS	HOLE SPACING				NTING OLES			
78"	72"	2	16''	15''	31''	47''	63"			
84''	72"	2	17''	161/2"	331/2"	501/2"	6 <b>7</b> 1/21			
90"	72"	2	18''	18''	36''	54''	72''			
96"	90"	2	19"	191/2"	381/2''	571/2"	761/21			
102"	90"	2	20"	21''	41''	61''	81''			
108''	90"	2	21''	221/21	' 43 <sup>l</sup> / <sub>2</sub> ''	641/2"	851/21	1		
114''	108''	3	15''	12''	2 <b>7</b> ''	42"	5 <b>7</b> "	<b>7</b> 2"	87"	102"
120''	108''	3	16''	12''	28''	44''	60"	76"	92"	108''
126"	108''	3	17''	12''	29"	46''	63"	80"	97"	114''
132"	126''	3	18''	12''	30"	48"	66"	84"	102"	120''
138''	126''	3	19''	12''	31''	50"	69"	88"	107''	126"
144''	126''	3	20"	12''	32"	52"	72"	92"	112''	132"



PLOT BY: mscj9h

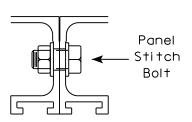


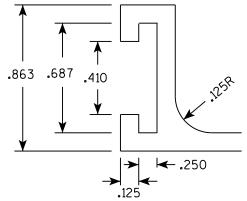
### STITCH BOLT, WASHER & NUT

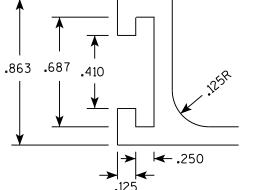
The hardware includes:

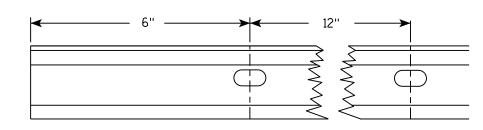
3/8 " - 16 X 3/4 " Economy Bolt 2024-T4 alloy 3/8 " - Stainless steel stop nut

3/8" X .064 Flat Washers, Alclad 2024-T4 alloy





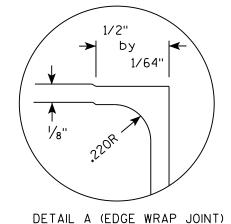




See Detail A

See Detail A

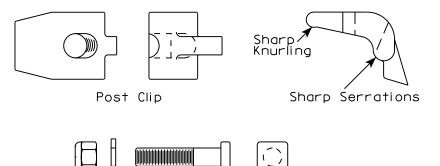
Punch 7/16" x 7/8" oval holes beginning 6" in from end of extrusion 12" CC on both edges of 6" and 12" panels.



PLOT BY: \$\$...plotuser...\$\$

POST CLIP, POST CLIP BOLT, WASHER & NUT

Post Clip shall be Alum. Alloy 356-T6 Post Clip Bolt shall be Stainless Steel. Flat washer shall be 3/8" X .091. Stainless Steel. Stop nut shall be stainless steel.



Post Clip Bolt



- 1. The contractor may select any brand of extrusion that conforms to the illustrations or meets with the approval of the engineer, but all extrusions used on this contract shall be of the same brand.
- 2. Panel Stitch Bolts shall be used to assemble adjacent panels. Maximum stitch bolt spacing shall be 24" C-C, and a minimum of 4 bolts shall be used to connect any two extrusions.

Flat Washer

Stop Nut

- 3. Post Clips shall be used to attach the sign panel to the sign support.
- 4. Edge wrapping of sign sheeting required on all extrusions ioints shown in Detail A.

ALUMINUM EXTRUSIONS FOR TYPE I SIGNS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED For State Traffic Engineer
DATE 11/30/16 PLATE NO. A5-2.10

SHEET NO:

PROJECT NO:

Ε

12" Extrusion

Minimum Weight

2.5 lb./ft.

Extruded Shape

**←.**125

Ы

→ | ← .125

6" Extrusion Minimum Weight 1.4 lb./ft.

See Detail A

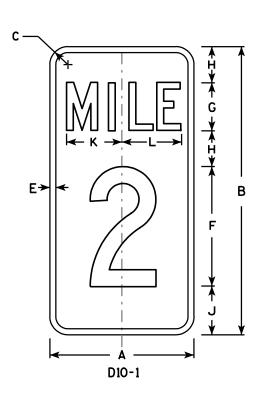
1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

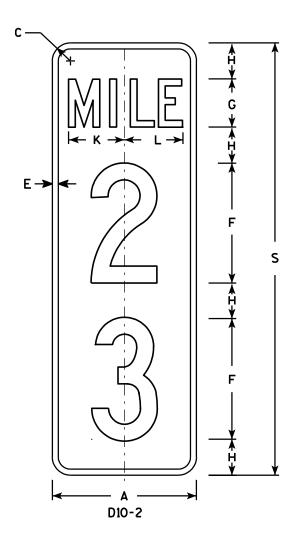
2. Color:

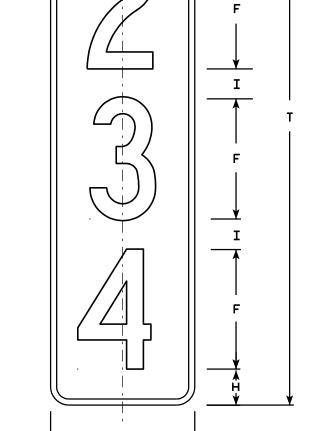
Background - Green

Message - White - Type H Reflective

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- Optically adjust numerals about the centerline of the sign to achieve proper balance.







D10-3

Metric equivalent for this sign is:

	11113 31911 13.
PHY. SIZE	
12 X 24	300 mm X 600 mm
12 X 36	300 mm X 900 mm
12 X 48	300 mm X 1200 mm

		•				•																				
SIZE	Α	В	С	D	Е	F	O	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z
1																										
2																										
3																										
4	12	24	1 1/2		1/2	10	4	3	2 1/2	4	4 5/8	4 1/8		·					36	48						
5	12	24	1 ½		1/2	10	4	3	2 1/2	4	4 %	4 1/8							36	48						
														,						_						

STANDARD SIGN D10-1 , D10-2 & D10-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J Spany
for Director, Office of Traffic

DATE 1/16/02 PLATE NO. D10-3.2

SHEET NO:

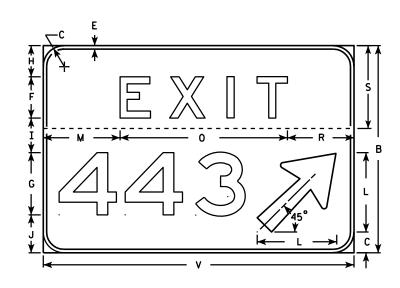
FILE NAME : C:\Users\Projects\tr\_stdplate\D103.DGN

PROJECT NO:

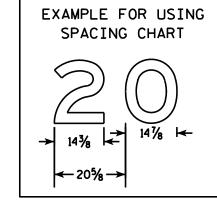
PLOT DATE: 28-SEP-2005 08:20

PLOT BY : DOTDZK

NOTE: T diminision shall be measured from the back of the number to the front of the arrow



HWY:



### NOTES

- 1. Sign is Type II Type H reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Green Message - White (Type H reflective)

- 3. Message Series E
- 4. Corners may be square or rounded but the border shall be rounded as shown.
- 5. Base material for this sign shall be plywood and shall be split into two seperate pieces as shown on each detail by the dashed line (-----).
- 6. Arrow is Type "A" from sign plate A1-1.
- 7. Substitute appropriate message, space per the table and adjust placement on sign to achieve proper balance.
- 8. As per the Standard Spec's, these signs shall not have a vertical joint.

### SPACING CHART FOR 18" NUMERALS

NUMBERS	WIDTH	0	1	2	3	4	5	6	7	8	9	A	В	С
0	14 1/8	21 %	21 %	21 1/4	20 ¾	21 1/4	21 1/4	21 %	19 %	22 1/8	21 %			
1	5 %	14	14	14	13 1/8	12 1/8	14	14	12 1/4	14	14			
2	14 3/8	20 %	21 1/8	20 %	19 ¾	20 1/4	20 %	20 %	18 1/8	21 1/8	20 %			
3	14 3/8	20 ¾	21 1/8	20 ¾	20 1/4	19 ¾	20 ¾	20 ¾	19 ¾	21 1/8	20 ¾			
4	16 5/8	22 1/8	22	22 1/2	21 %	21 %	22 1/8	22 1/8	20 %	23 3/8	22 1/8			
5	14 1/4	20 %	20 %	20 1/8	18 ¾	18 ¾	20 %	20 %	18 3/8	20 %	20 %			
6	14 1/2	21 1/4	20 ¾	20 ¾	19 1/8	19 1/2	20 ¾	20 ¾	19	20 ¾	20 ¾			
7	14 3/8	19 ¾	19 ¾	20 1/4	19 ¾	17	19 ¾	20 %	18	20 %	19 ¾			
8	14 1/4	21 1/2	21 1/2	20 %	19 1/4	19 ¾	20 %	21 1/2	19 %	21 1/2	20 %			
9	14 1/2	21 1/4	21 1/4	20 1/8	20	20 %	20  %	21 1/4	19 1/2	21 ¾	21 1/4			
A	18	19 ¾	19 ¾	20 1/4	19 ¾	17	19 ¾	20 %	18 1/8	20 %	19 ¾	21 5/8	23 1/8	23
В	14 1/4	21 1/2	21 1/2	20 %	19 1/4	19 ¾	20 %	21 1/2	19 %	21 1/2	20 %	19 1/2	22 3/8	21 1/2
С	14 1/4	21 1/4	21 1/4	20 1/8	20	20 %	20  %	21 1/4	19 1/2	21 ¾	21 1/4	18 3/8	21	19 ¾

SIZE	A	В	С	D	Ε	F	G	Н	ı	J	K	L	М	N	0	Р	0	R	S	T	U	v	W	x	Y	Z
2																										
3																										
4																										
5	72	60	6		1	12	18	9	10	11	8 %	23	22 1/4	13 1/4	48 1/2	10 1/4		19 1/4	24	7		90		96	7 1/2	

COUNTY:

Sq. Ft. 30 37.5 STANDARD SIGN E5-1A

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer DATE 5/11/10 PLATE NO. E5-14.12

SHEET NO:

PROJECT NO:

PLOT DATE: 11-MAY-2010 15:32

PLOT NAME :

PLOT BY : dotsja

PLOT SCALE: 27.808870:1.000000

WISDOT/CADDS SHEET 42

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\E51A.DGN

- 1. Sign is Type II Type F Reflective
- 2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance

C —		<b>A</b> A
D->  <del>-</del> E->   <del>-</del>		H → F → H B
	NEXT O MILES	F G J
•	A —	<u> </u>
	G20-1	

SIZ	Ε Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	Q	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2	60	24	1 3/8	1/2	5/8	6	4 ½	3 ¾		16 ¾	18 1/2	3		16	18 %												10
3																											
4	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4		16 3/4	18 1/2	3		16	18 %												10
5																											

COUNTY:

STANDARD SIGN G20-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew & Rauch

For State Traffic Engineer
DATE 3/14/17 PLATE NO. G20-1.8

SHEET NO:

PROJECT NO:

FILE NAME: C:\CAEfiles\Projects\tr\_stdplate\G201.DGN

HWY:

PLOT DATE: 14-MAR-2017 13:28

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

PLOT SCALE : 6.889165:1.000000

WISDOT/CADDS SHEET 42

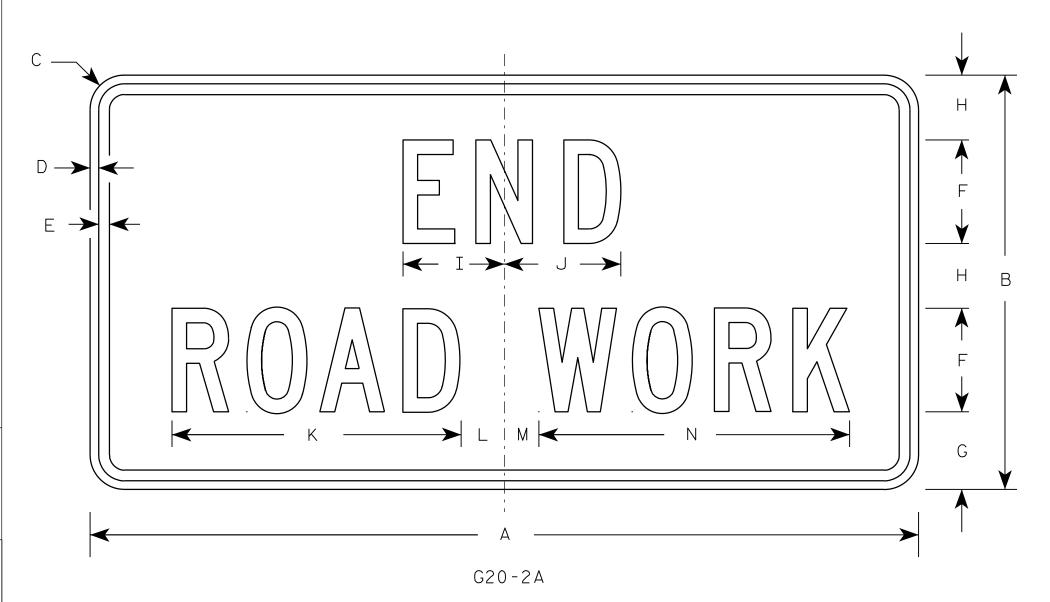
| "

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Metric equivalent for this sign is:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 1/8	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72

COUNTY:

STANDARD SIGN G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED 400 110 00 00 110

for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\G202A.DGN

HWY:

PROJECT NO:

PLOT DATE: 30-SEP-2009 09:31

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE : 5.561773:1.000000

5.561773:1.000000 WISDOT/CADDS SHEET 42

- 1. Sign is Type II See Note 6 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Top Red - Bottom Blue (See Note 6) Message - White - See Note 6

- 3. Message Series See note 5
- 4. Substitute appropriate numerals & ajust spacing as per plate A10-1.
- 5. M1-1 Numerals D Interstate - C

M1-1A - All copy - C

6. Permanent Signs

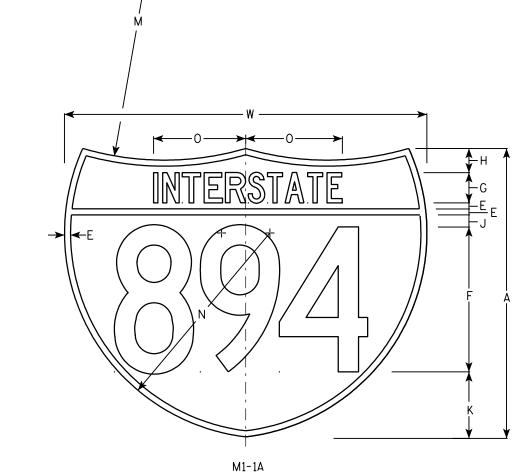
Message - Type H Reflective

Detour or other temporary signs Background - Reflective

Message - Reflective

M1-1

HWY:



PLOT DATE: 13-OCT-2005 14:49

Metric equivalent for these signs are:

SIZE	M1-1	SIZE	M1-1A
1			
2	600 mm X 600 mm	2	600 mm X 750 mm
3	900 mm X 900 mm	3	900 mm X 1125 mm
4	900 mm X 900 mm	4	900 mm X 1125 mm
5	900 mm X 900 mm	5	900 mm X 1125 mm

<u></u>	100	, ,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	X 300	וווווו		J00 I	IIIII X I	123 11111	<u>'                                    </u>																	M1-1	W1-1A	W1-1	W1-1A
SIZE	Α	В	С	D	E	F	G	Ι	I	J	K	L	М	N	0	Ρ	a	R	S	T	U	٧	W	Х	Y	Area sq. ft.	Area sq. ft.	Area m2	Area m2
1																													
2	24				1/2	12	2 ½	2		1	5 ½	15	24	17	7 1/8								30			3 <b>.</b> 13	3.91	.36	.46
3	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 ½	36	25 ½	11 ¾								45			7.03	8.79	<b>.</b> 81	1.05
4	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 ½	36	25 ½	11 ¾								45			7.03	8.79	.81	1.05
5	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 ¾								45			7.03	8.79	<b>.</b> 81	1.05

COUNTY:

INTERSTATE ROUTE MARKER M1-1 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

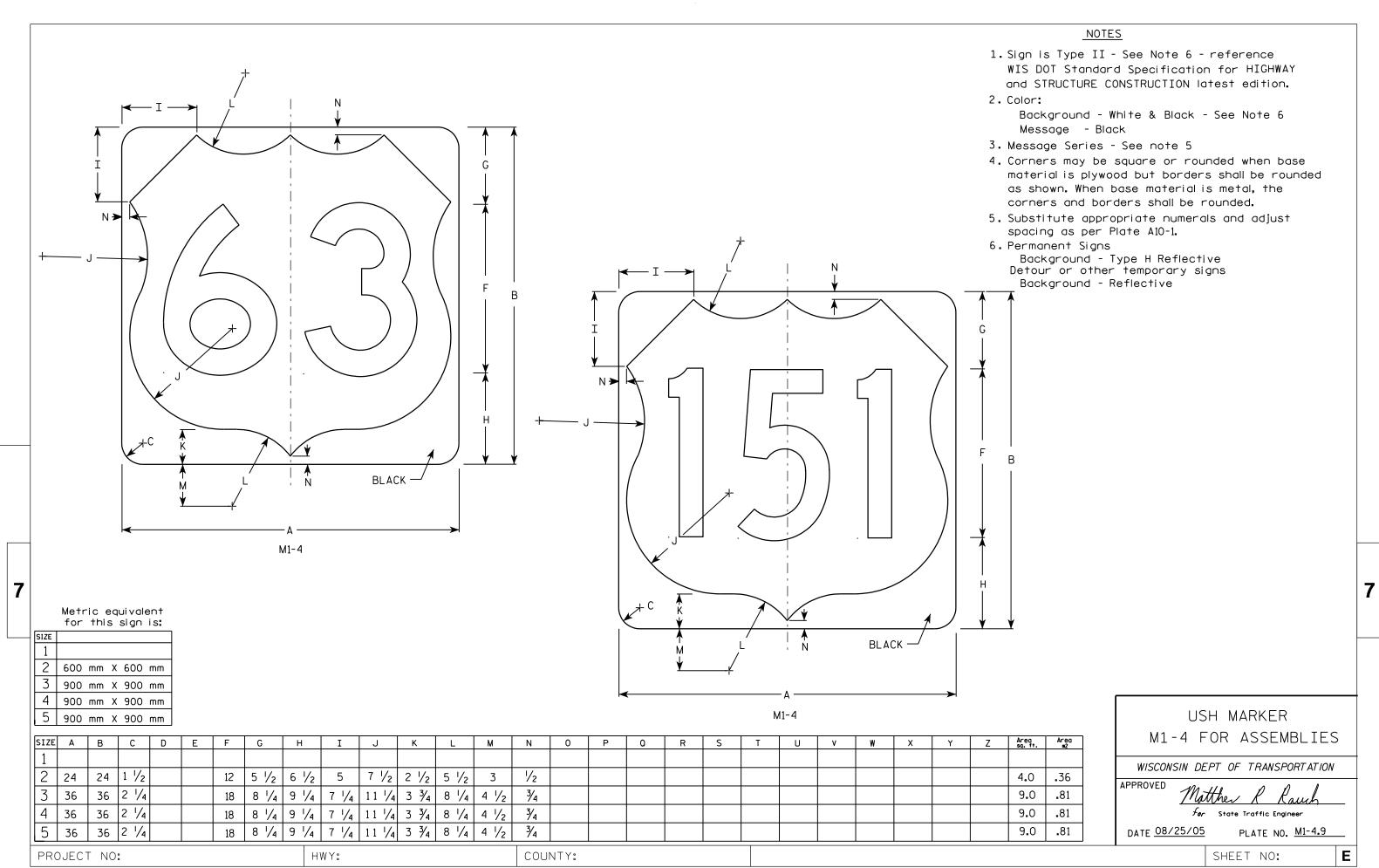
DATE 08/23/05

For State Traffic Engineer

SHEET NO:

PLOT BY : DITJPH PLOT NAME :

PROJECT NO:



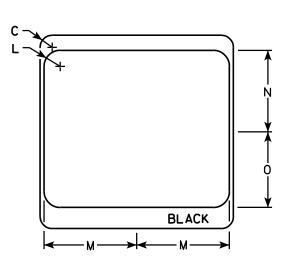
FILE NAME : C:\Users\Projects\tr\_stdplate\M14.DGN

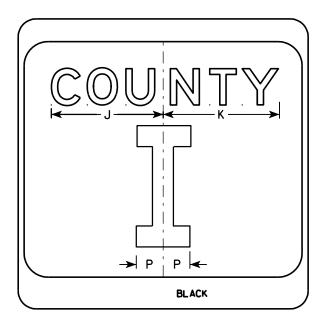
- 1. Sign is Type II see Note 7 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

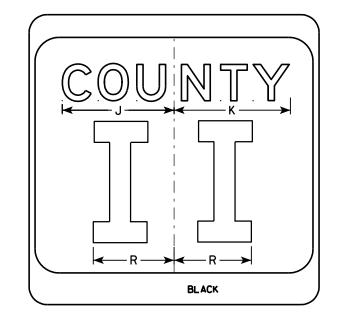
Background - White & Black - See Note 7 Message - Black

- 3. Message Series see Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Message Series E for 1 letter. Message Series D for 2 letters unless message is too big then Series C. Message Series C for 3 letters unless message is too big then Series B.
- 6. Substitute appropriate letters & optically center to achieve proper balance.
- 7. Permanent Signs

Background - Type H Reflective Detour or temporary Signs Background - Reflective







SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 %	2	11 1/2	10 1/8	9 %	2 1/4		6 %									4.0
3	36		2 1/4			16	4	7 5/8	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 %		10									9.0
5	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
																			_								

COUNTY:

CTH MARKER M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

FerState Traffic Engineer PLATE NO. M1-5A.8 DATE 9/27/11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\M15A.DGN

PROJECT NO:

**BLACK** 

HWY:

M1-5A

PLOT DATE: 29-SEP-2011 11:25

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 5.959043:1.000000

- 1. Sign is Type II See Note 6 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White & Black - See Note 6 Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
- 6. Permanent Signs Background - Type H Reflective Detour or temporary Signs Background - Reflective

	F A  BLACK
Metric equivalent for this sign is:	<b>&gt;</b>

HWY:

SIZE 600 mm X 600 mm 900 mm X 900 mm 900 mm X 900 mm 900 mm X 900 mm

PROJECT NO:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	Т	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 %	11 1/2	1	1 1/8	11 1/4	21 1/8											4.0	<b>.</b> 36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 %	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	<b>.</b> 81

COUNTY:

STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 3/20/02 PLATE NO. M1-6.9

SHEET NO:

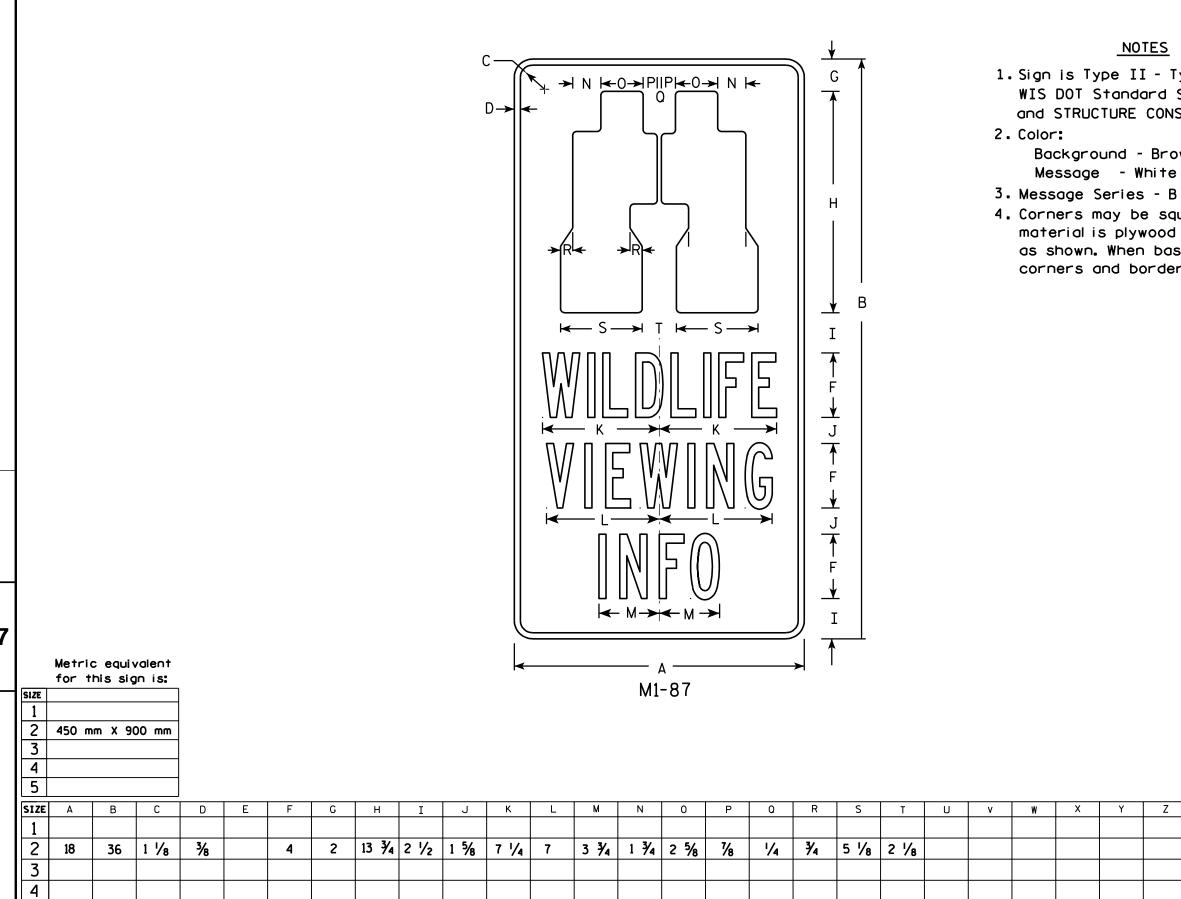
FILE NAME : C:\Users\Projects\tr\_stdplate\M16.DGN

PLOT DATE: 13-OCT-2005 14:55

PLOT BY : DITJPH

PLOT NAME :

PLOT SCALE : 6.715871:1.000000



COUNTY:

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

Background - Brown Message - White - Type H Reflective

- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

Area sq. ft.

4.5

.41

STANDARD SIGN M1 - 87

WISCONSIN DEPT OF TRANSPORTATION

APPROVED DATE 1/30/02 PLATE NO. M1-87.3

SHEET NO:

HWY:

5

PROJECT NO:

PLOT BY : DITJPH

# 

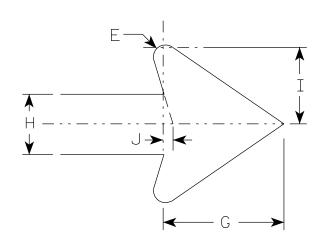
M1-88A

### NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Brown Message - White - Type H Reflective

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Metric equivalent for this sign is:

				_	
SIZE					
1					
2	300	mm	X	300	mm
3					
4					
5					

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1																												
2	12		1 1/8				3 3/4	1 1/8	2 3/8	5/16																	1.0	.09
3																												
4																												
5																												
DDO	JECT	NO.					Ш	WY:					Lcou	NTY:					Т									
L K O	ノニしょ	NO.					1 11	VV I .						IN I I .														

STANDARD SIGN M1-88A

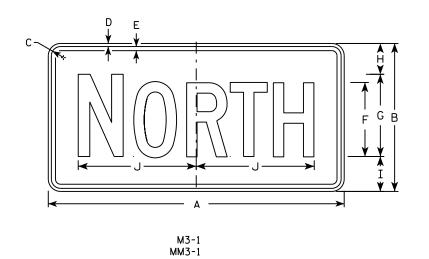
WISCONSIN DEPT OF TRANSPORTATION

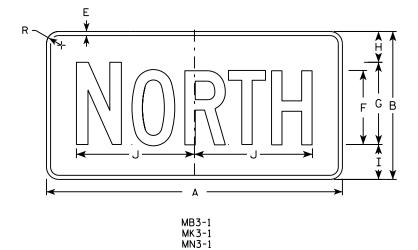
DATE 1/30/02 PI

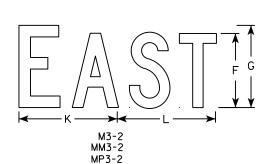
30/02 PLATE NO. M1-88A.2

SHEET NO:

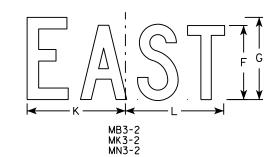
PLOT BY : DOTSJA

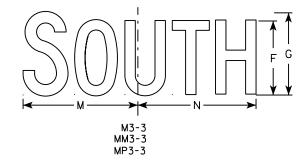


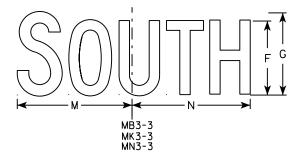


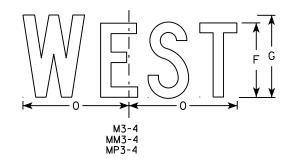


MP3-1

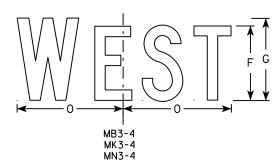








HWY:



### NOTES

- 1. All Signs Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

5. M3-1 thru M3-4 Background - White Message - Black

MB3-1 thru MB3-4 Background - Blue

Message - White

MK3-1 thru MK3-4 Background - Green

Message - White

MM3-1 thru MM3-4 Background - White

Message - Green

MN3-1 thru MN3-4 Background - Brown

Message - White

MP3-1 thru MP3-4 Background - White

Message - Blue

6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 1/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

COUNTY:

STANDARD SIGNS M3-1 thur M3-4 **SERIES** 

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 10/15/15 PLATE NO. M3-1.14

Ε

SHEET NO:

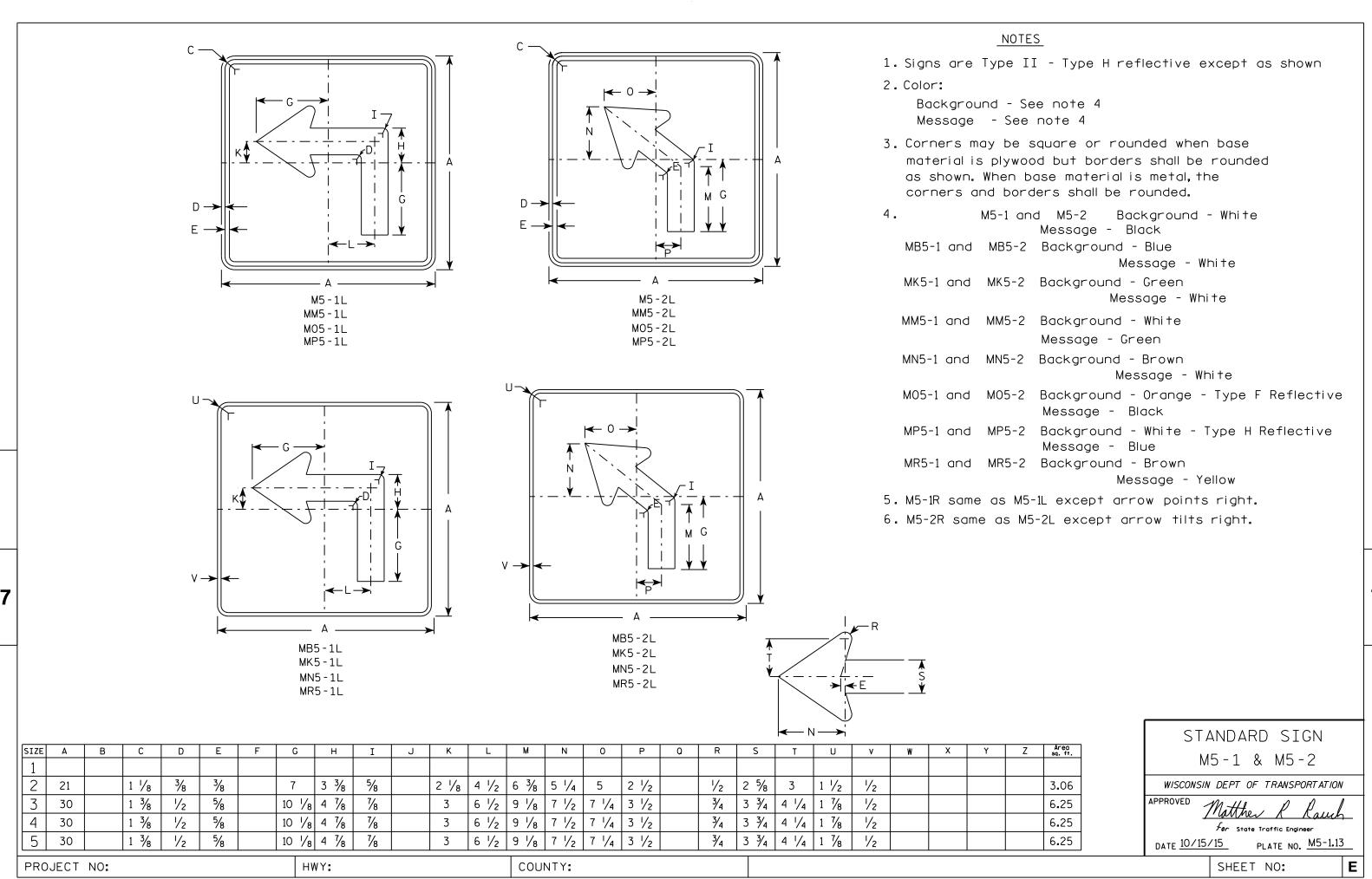
FILE NAME · C·\CAFfiles\Projects\tr stdolote\M31 DCN

PROJECT NO:

PLOT DATE . 01-DEC-2015 17:54

PLOT RY . \$\$ plotuser \$\$ PLOT NAME :

PLOT SCALE . 11 675051.1 000000

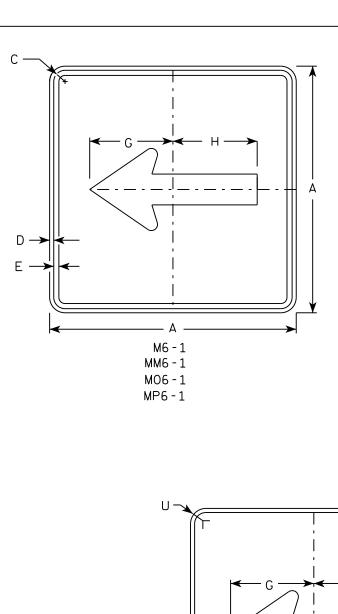


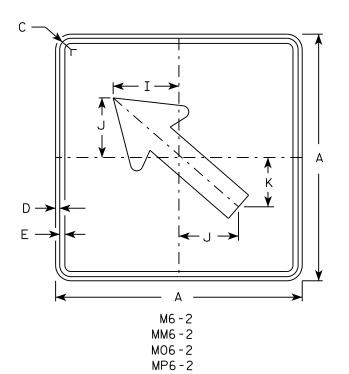
FILE NAME . C.\CAFfiles\Projects\tr stdolote\M51 DCN

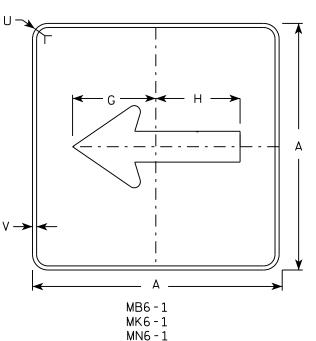
PLOT DATE . 01-DEC-2015 18:07

PINT RY . \$\$ DIOTUSET \$\$ PINT NAMF :

PLOT SCALE . 11 675051.1 000000

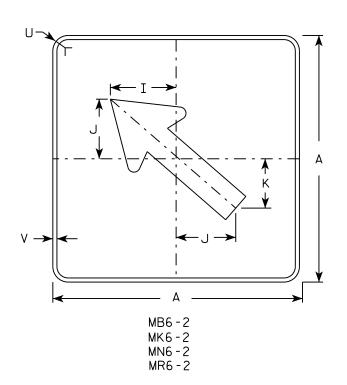






MR6-1

HWY:



### NOTES

- 1. Signs are Type II Type H except as Shown
- 2. Color:

Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background White

Message - Black

MB6-1 and MB6-2 Background - Blue

Message - White

MK6-1 and MK6-2 Background - Green

Message - White

MM6-1 and MM6-2 Background - White

Message - Green

MN6-1 and MN6-2 Background - Brown

Message - White

M06-1 and M06-2 Background - Orange - Type F Reflective

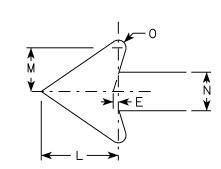
Message - Black

MP6-1 and MP6-2 Background - White

Message - Blue

MR6-1 and MR6-2 Background - Brown

Message - Yellow



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	٥	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 %	5	4 1/4	5 1/4	3	2 %	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthe R Raul

Ε

DATE 10/15/15 PLATE NO. M6-1.15

SHEET NO:

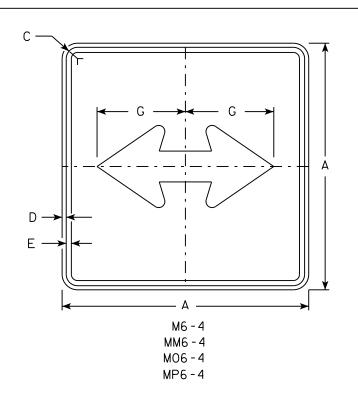
FILE NAME · C·\CAFfiles\Projects\tr stdplate\M61 DCN

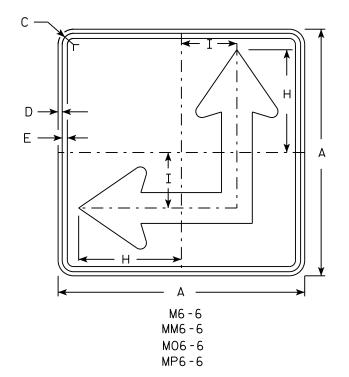
PROJECT NO:

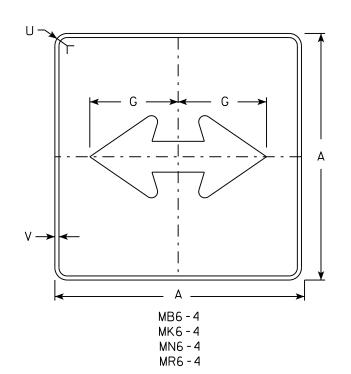
PLOT DATE . 01-DEC-2015 17:57

PINT RY . \$\$ plotuser \$\$ PINT NAMF :

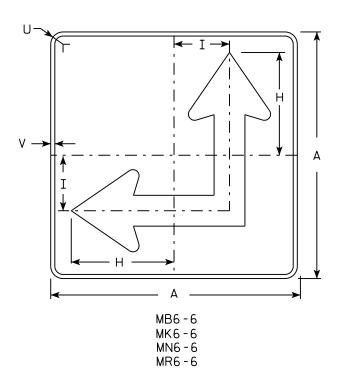
PLOT SCALE . 11 675051.1 000000







HWY:



### NOTES

- 1. Signs are Type II Type H except as Shown
- 2. Color:

Background - See Note 4 Message - See Note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-4 and M6-6 Background White Message - Black

MB6-4 and MB6-6 Background - Blue

Message - White

MK6-4 and MK6-6 Background - Green

Message - White

and MM6-6 Background - White MM6-4

Message - Green

MN6-4 and MN6-6 Background - Brown

Message - White

M06-4 and M06-6 Background - Orange - Type F Reflective

Message - Black

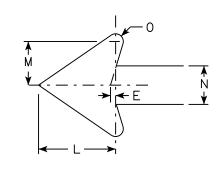
MP6-4 and MP6-6 Background - White

Message - Blue

MR6-4 and MR6-6 Background - Brown

Message - Yellow

5. M6-6R same as M6-6L except arrow points ahead and right.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	a	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	8 3/4	4 1/4			5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
																											==

COUNTY:

STANDARD SIGN M6-4 & M6-6 SERIES

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

APPROVED

DATE 10/15/15

PLATE NO. M6-4.10 Ε

PLOT DATE . 01-DEC-2015 17.58

PLOT RY . \$\$ plotuser \$\$ PLOT NAME :

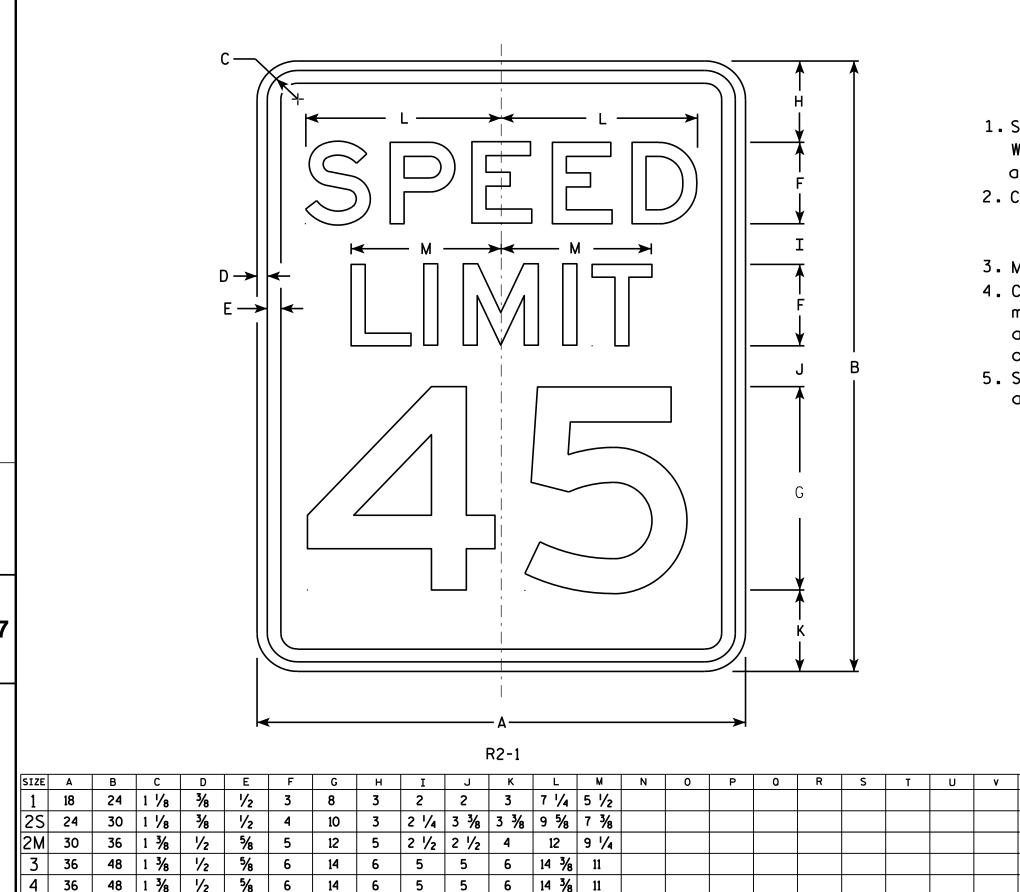
PLOT SCALE . 11 675051.1 000000

NOTES 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. 2. Color: Background - Red Message - White 3. Message Series - C R1-1 SIZE A STANDARD SIGN 30 5/8 10 12 1/2 45° 12 3/4 5.18 2S 30 5/8 12 1/2 45° 12 3/4 10 5.18 R1-1 2M 36 3/4 12 15 45° 15 % 7.46 3/4 15 3/8 12 45° 36 15 7.46 WISCONSIN DEPT OF TRANSPORTATION 45° 20 1/2 48 16 20 13.25 APPROVED Matthew & Kauch 5 48 16 20 45° 20 1/2 13.25 3/8 7 3/4 45° 7 3/4 1.86 18 6 For State Traffic Engineer 12 1/4 4 45° 5 1/8 0.78 DATE <u>11/12/15</u> PLATE NO. \_\_\_\_\_R1-1.13 COUNTY: SHEET NO: PROJECT NO: HWY: PLOT SCALE • 4 378143•1 000000

FILE NAME · C·\CAFfiles\Projects\tr stdplote\R11 DGN

PLOT DATE . 01-DEC-2015 18:07

PINT RY . \$\$ plotuser \$\$ PINT NAMF :



4 1/2 6 3/4 6 3/4 19 1/4 14 5/8

COUNTY:

20

HWY:

6

### NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal. the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

3.0

5.0

7.5

12.0

12.0

20.0

STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION APPROVED

Matther R Raus For State Traffic Engineer PLATE NO. R2-1.13

DATE <u>5/26/1</u>0

SHEET NO:

2 1/4

60

5

48

PROJECT NO:

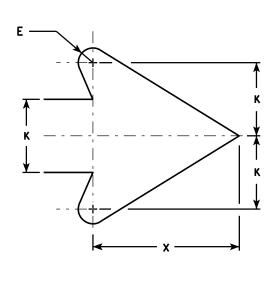
PLOT NAME :



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

													R3-4B	1													
SIZE	A	В	С	D	Е	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	Х	Y	Z	Areg sq. ft.
1																											
2S																											
2M																											
3																											
4	36	48	1 5/8	5/8	3/4	1 3/4	7 1/8	19 7/8	6 3/4	2 3/4	3 3/4	7 1/8	15 3/4	45°	8 %	2		13 %	4 5/8	7 3/4	1 1/8	14 1/4	5 1/2	7 5/8			12.0
5	36	48	1 5/8	5/8	₹4	1 3/4	7 1/8	19 7/8	6 3/4	2 3/4	3 3/4	7 1/8	15 3/4	45°	8 %	2		13 %	4 %	7 3/4	1 1/8	14 1/4	5 1/2	7 5/8			12.0

COUNTY:

STANDARD SIGN R3-4B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED M

₹or State Traffic Engineer

DATE 3/17/2011

PLATE NO. R3-4B.2

SHEET NO:

HWY:

PROJECT NO:

PLOT NAME :



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE A В С G 2S 24 3/8 10 6 1/4 6 3/4 7 1/8 7 5/8 30 | 1 1/8 1/2 | 3 % | 2 1/4 | 9 ¾ | 5.0 3 % 2 1/4 9 3/4 10 6 1/4 6 3/4 7 1/8 7 5/8 1/2 5.0 24 30 | 1 1/8 48 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 5/8 3/4 15 9 3/8 14 % 10 10 3/4 11 3/8 12.0 36 10 3/4 11 3/8 3/4 15 9 % 10 36 1 % 12.0 48 5 7 1/4 4 1/2 19 1/2 20 12 1/2 13 1/2 14 1/4 15 1/4 48 60 2 1/4 20.0

STANDARD SIGN R4-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

WED Matthew & Rauch

State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-3.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\R43.DGN

PROJECT NO:

PLOT DATE: 25-MAR-2011 13:40

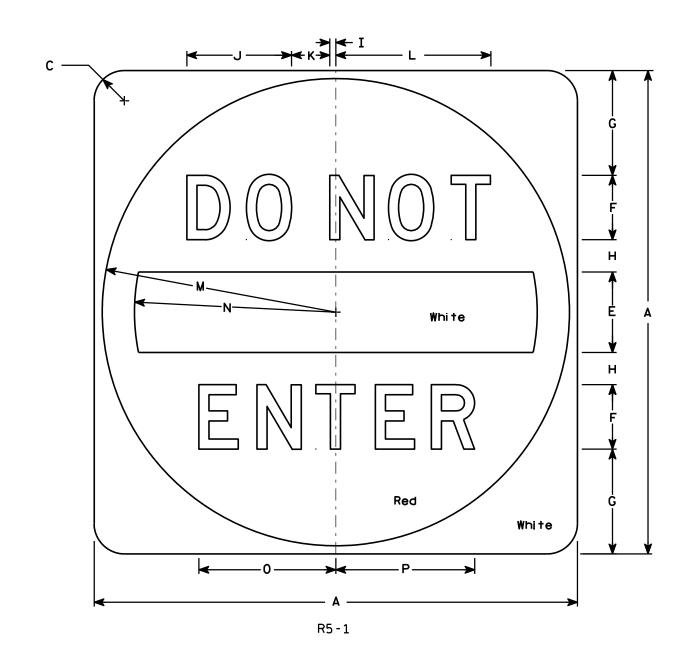
PLOT BY: mscsja

# <u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See detail Message - White - Type H Reflective

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but when base material is metal, the cornors shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1																											
25	30		1 1/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 %	14 1/2	12 1/2	8 1/2	8 %											6.26
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 ¾											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 %	14 1/2	23 ½	20	12 3/4	12 1/8											16.0

COUNTY:

STANDARD SIGN R5-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

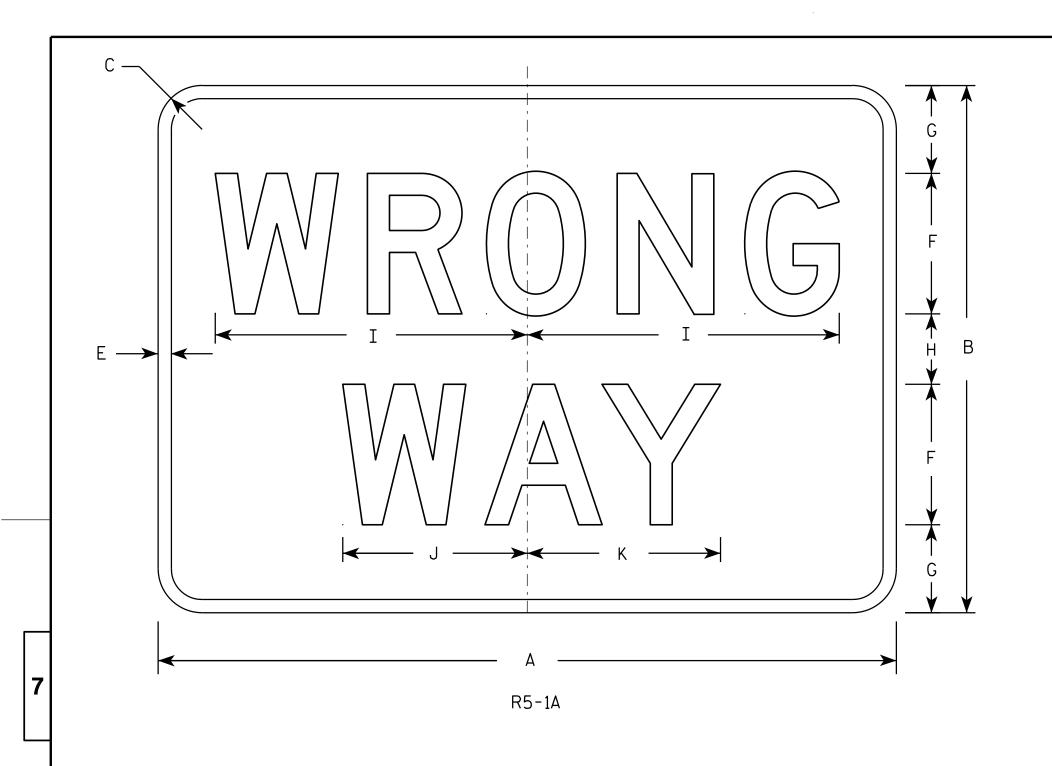
DATE 12/17/10

10 PLATE NO. R5-1.15

Р

PLOT NAME :

HWY:



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Red Message - White

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	Α	В	С	D	Ε	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	30	18	1 1/2		1/2	5	3	2	11	6 ½	6 %																3.75
2S	36	24	2		5/8	6	4 1/2	3	13 1/4	7 %	8 1/4																6.00
2M	42	30	2 1/2		3/4	8	5	4	17 ¾	10 1/2	11																8.75
3	42	30	2 1/2		3/4	8	5	4	17 3/4	10 1/2	11																8.75
4	42	30	2 1/2		3/4	8	5	4	17 3/4	10 1/2	11																8.75
5	42	30	2 1/2	·	3/4	8	5	4	17 3/4	10 1/2	11	·		·													8.75

COUNTY:

STANDARD SIGN R5-1A

WISCONSIN DEPT OF TRANSPORTATION

Matther R Raud PLATE NO. R5-1A.2

DATE 12/17/10

SHEET NO:

PROJECT NO:

HWY:

PLOT BY: dotsja

PLOT NAME :

# 

Red

R5-1C

# **NOTES**

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See detail Message - White - Type H Reflective

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but when base material is metal, the cornors shall be rounded.

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	M	N	0	Р	0	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 1/8		5	4	6 1/2	2	3⁄8	6 ½	2 3/8	9 5/8	14 1/2	12 1/2	8 1/2	8 %		45°									5.18
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 ¾		45°									7.46
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 ¾		45°									7.46
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 ¾		45°									13.25
5	48		3		8	6	11	3	5/8	9 3/4	3 %	14 1/2	23 1/2	20	12 3/4	12 1/8		45°									13.25
PRO	JECT	NO:					нм	/Y <b>:</b>					COUN	ITY:													

STANDARD SIGN R5-1C

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

State Traffic Engineer

DATE 3/23/11

11 PLATE NO. R5-1C.1
SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_misc\R51clipped.dgn

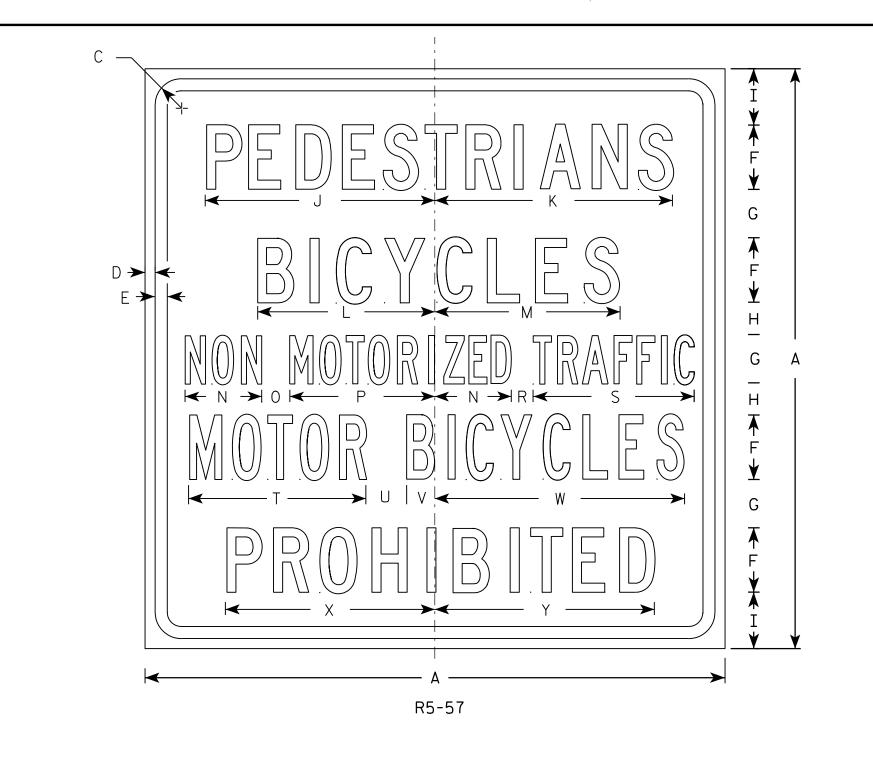
PLOT DATE: 23-MAR-2011 13:26

Whi te

PLOT NAME :

PLOT BY: mscj9h

PLOT SCALE: 5.959043:1.000000



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series Lines 1, 2, and 5 are Series C. Lines 3 and 4 are Series B.
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE 2M 3 4 5/8 3 1/2 14 1/4 14 7/8 11 11 1/2 4 3/4 1 3/4 1 3/8 2 1/2 1 3/4 15 1/2 13 36 1 5/8 3/4 3 1 1 13 % 9.0 4 11 1/2 4 3/4 1 3/4 5 3  $3 \frac{1}{2} | 14 \frac{1}{4} | 14 \frac{7}{8} | 11$  $1\frac{3}{8}$ 2 1/2 1 3/4 | 15 1/2 36 2 13 9.0 11

COUNTY:

STANDARD SIGN R5-57

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauh

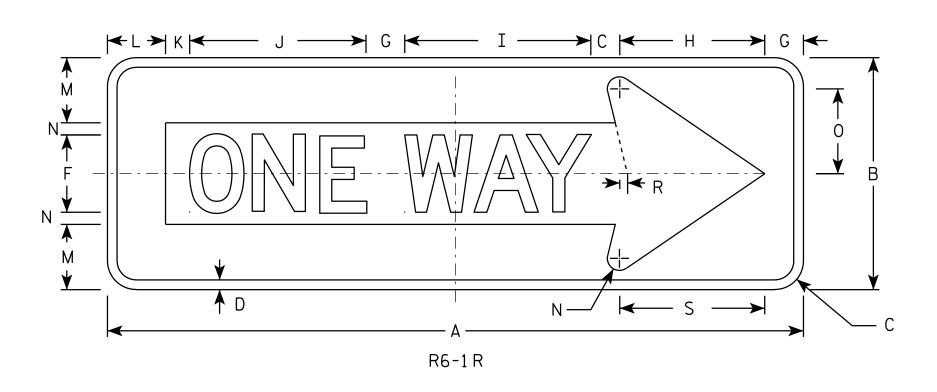
DATE 3/29/2011 PLATE NO. R5-57.10

SHEET NO:

PROJECT NO:

HWY:

PLOT NAME :



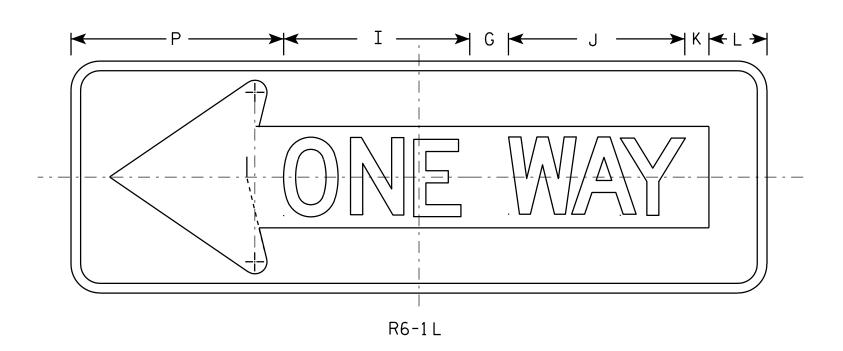
### <u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - BLACK

Message - BLACK LEGEND & WHITE ARROW & BORDER

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Arec sq. f
1																											
25	36	12	1 1/2	1/2		4	2	7 1/2	9 %	9 1/8	1 1/4	3	3 %	5/8	4 3/8	11		3/8	7 1/2								3.0
2M	54	18	2 1/4	3/4		6	3	11 1/4	14 1/2	13 %	1 1/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.
3	54	18	2 1/4	3/4		6	3	11 1/4	14 1/2	13 %	1 1/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.
4	54	18	2 1/4	3/4		6	3	11 1/4	14 1/2	13 %	1 1/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.
5																											

STANDARD SIGN R6-1 L & R

WISCONSIN DEPT OF TRANSPORTATION

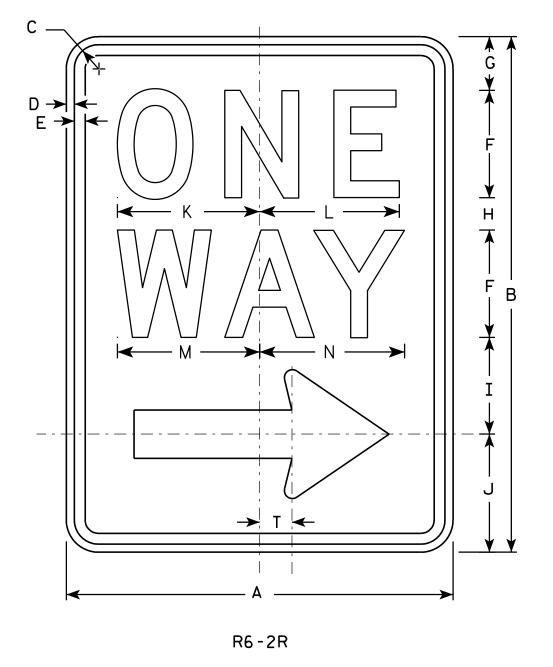
APPROVED

Matther & Kaure For State Traffic Engineer

DATE 12/17/10

O PLATE NO.R<u>6-1.2</u> SHEET NO:

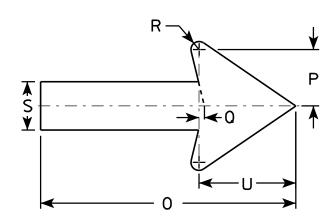
PLOT DATE: 17-DEC-2010 14:11 PLOT BY: dotsja



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. R6-2L same as R6-2R except arrow points to the left.



SIZE	A	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z
1	18	24	1 1/8	3/8	1/2	5	2 1/2	1 1/2	4 1/2	5 ½	6 %	6 1/2	6 %	6 3/4	11 1/8	2 %	1/4	3/8	2 1/4	1 1/2	4 1/2					
2S	24	30	1 1/8	3/8	1/2	6	3	2 1/2	5 1/2	7	8 1/8	8 1/8	8 1/2	8 %	16	3 1/2	3/8	1/2	3	2	6					
2M	30	36	1 3/8	1/2	5/8	8	2 1/2	2	6 %	8	10 1/2	10 1/2	11 1/4	11 1/4	20	4 3/8	1/2	5/8	3 3/4	2 1/2	7 1/2					
3	36	48	1 1/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 %	1/2	3/4	4 3/4	3	9					
4	36	48	1 1/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 %	1/2	3/4	4 3/4	3	9					
5																										

COUNTY:

STANDARD SIGN R6-2 R&L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 11/2/10

PLATE NO. R6-2.8 SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\R62.DGN

HWY:

PROJECT NO:

PLOT DATE: 02-NOV-2010 15:25

PLOT BY: ditjph

PLOT NAME :

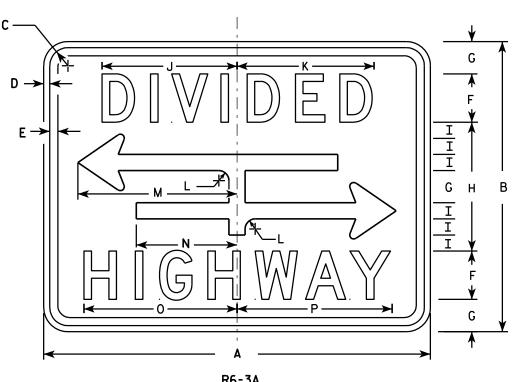
PLOT SCALE: 4.469282:1.000000

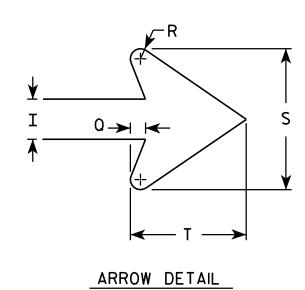
# <u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.





		_	
D٤	_	₹	٨

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	24	18	1 1/8	3/8	3/8	3	2	8	1	8 3/8 8	3 1/2	5/8	9 %	6 1/4	9 1/2	9 %	3∕8	1/4	3 1/2	2 3/4							3.0
2S	30	24	1 1/8	3/8	1/2	4	2 5/8	10 ¾	1 3/8	10 1/2 1	10 %	<b>1</b> / <sub>8</sub>	12 1/2	7 1/8	12 1/4	12 3/8	1/2	3/8	4 %	3 %							5.0
2M	30	24	1 1/8	3/8	1/2	4	2 %	10 ¾	1 3/8	10 1/2 10	10 %	%	12 1/2	7 1/8	12 1/4	12 3/8	1/2	3/8	4 %	3 %							5.0
3																											
4																											
5								·																		·	

STANDARD SIGN R6-3 & R6-3A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

for State Traffic Engineer DATE 3/31/2011 PLATE NO. R6-3.5

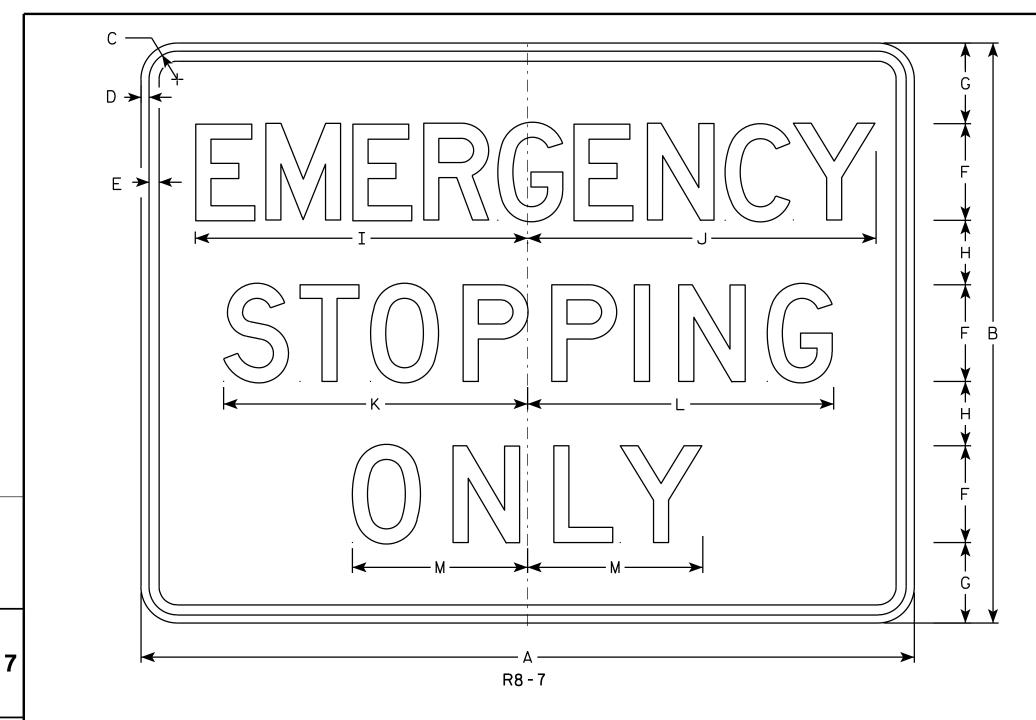
SHEET NO:

5.959043:1.000000

R6-3

PLOT DATE: 31-MAR-2011 09:08

PLOT BY: mscsja



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

STANDARD SIGN R8-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther & Rauch

DATE 3/31/2011

DII PLATE NO. R8-7.6
SHEET NO:

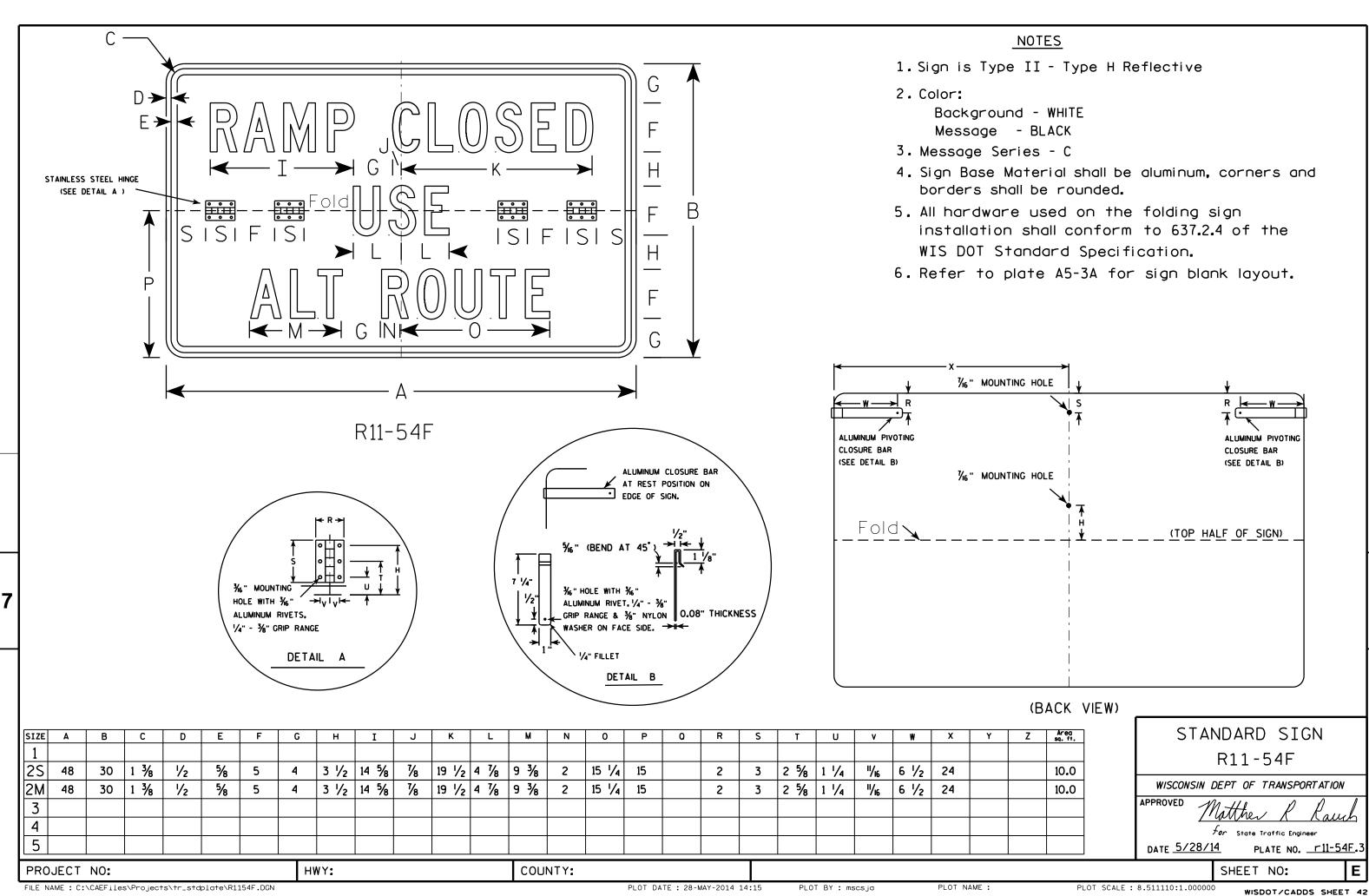
FILE NAME : C:\Users\PROJECTS\tr\_stdplate\R87.DGN

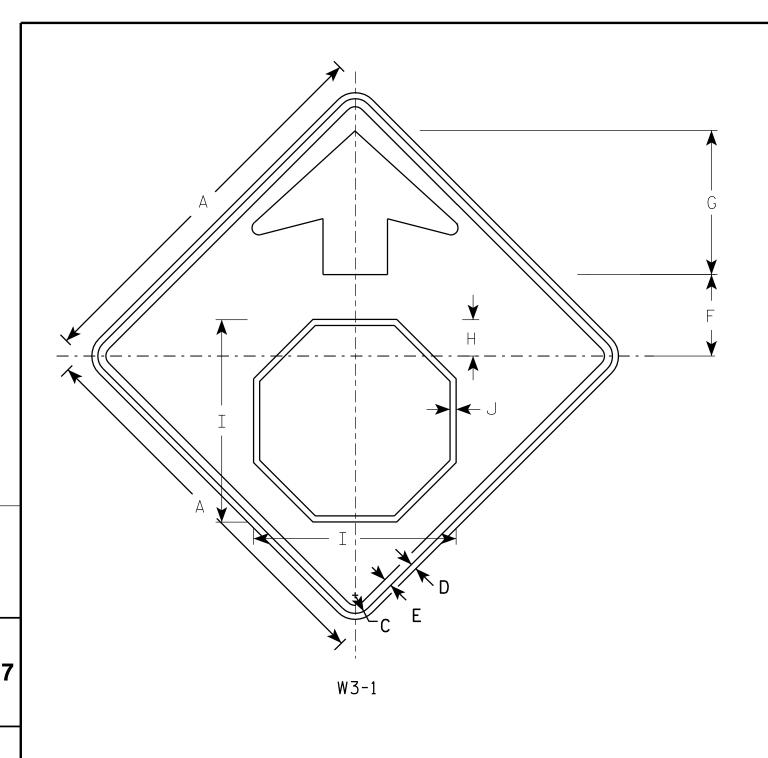
PLOT DATE: 31-MAR-2011 14:28

PLOT BY: mscsja

PLOT NAME :

PLOT SCALE: 5.959043:1.000000



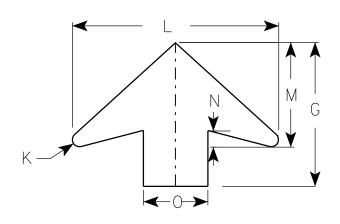


- 1. All Signs Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - YELLOW

Arrow & Border - BLACK

Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW	DFTAII
$\neg \cdots $	

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Ρ	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 1/8	15 ¾	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 %	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 %	6												9.0
2M	36		1 %	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 %	6												9.0
3	36		1 1/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 %	6												9.0
4	48		2 1/4	3/4	1	10	17 1/8	4 1/2	25 1/8	3/4	<b>7</b> ⁄8	25 %	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 1/8	4 1/2	25 1/8	₹4	<b>7</b> /8	25 %	13	2	8												16.0

STANDARD SIGN W3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew

For State Traffic Engineer

DATE 6/7/10 PLATE NO. W3-1.12

SHEET NO:

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W4-1L is the same as W4-1R except the arrow is reversed along the vertical centerline.

**⊬**L¦→

W4-1R

HWY:

SIZE	Α	В	С	D	Е	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	د	v	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	11 5/8	2 1/2	5	13	11	9	4 3/8	5 1/4	45°	3	8 %	9 1/2	3/4									6.25
25	36		1 %	5/8	3/4	14	2 3/4	6	15 ¾	13 1/4	10 1/4	5 1/4	6 3/8	45°	3 %	10 %	11 3/8	<b>1</b> / <sub>8</sub>									9.0
2M	36		1 %	5/8	3/4	14	2 3/4	6	15 ¾	13 1/4	10 1/4	5 1/4	6 3/8	45°	3 %	10 %	11 3/8	<b>½</b>									9.0
3	36		1 %	5/8	3/4	14	2 3/4	6	15 ¾	13 1/4	10 1/4	5 1/4	6 %	45°	3 %	10 %	11 3/8	<b>1</b> /8									9.0
4	48		2 1/4	3/4	1	18 3/4	3 %	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4									16.0
5	48		2 1/4	3/4	1	18 3/4	3 %	8	20 1/2	17 1/2	14 3/8	7	8 3/8	45°	4 3/4	14 1/4	15 1/4	1 1/4									16.0

COUNTY:

STANDARD SIGN W4-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Mathew Rauw

For State Traffic Engineer

DATE 03/12/13 PLATE NO. W4-1.14

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\W41.DGN

PROJECT NO:

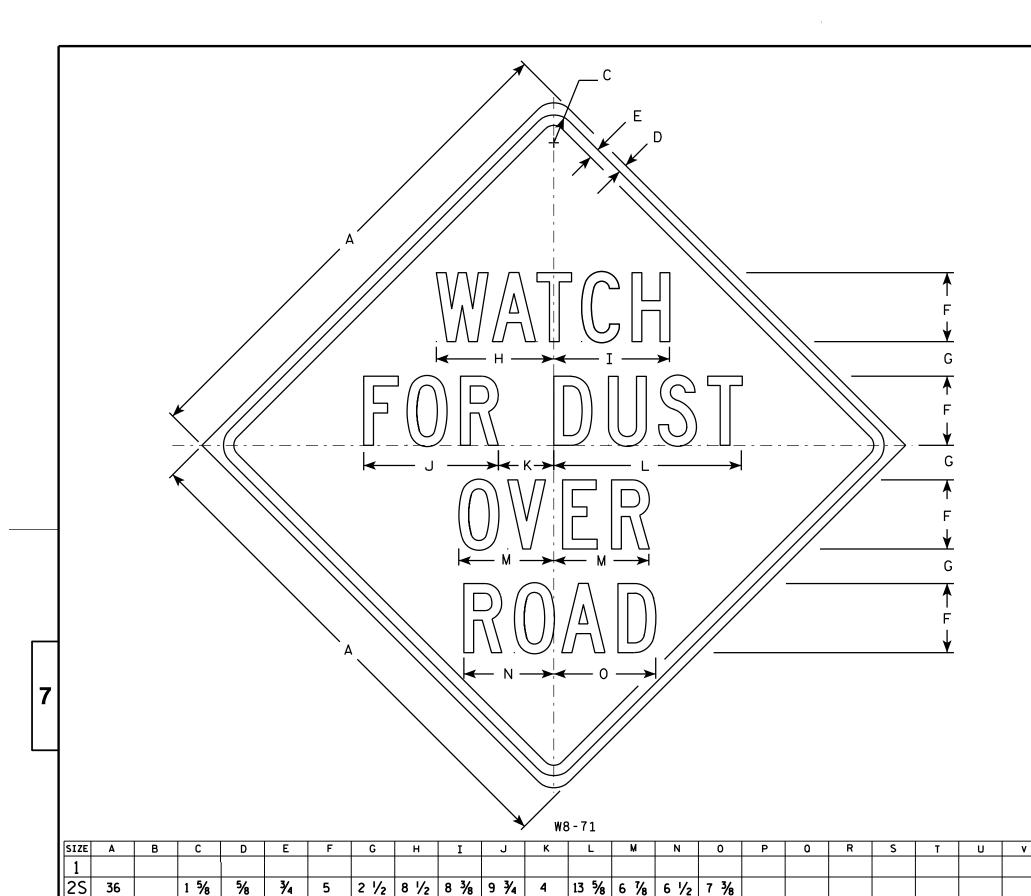
PLOT DATE: 12-MAR-2013 11:06

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE : 6.202372:1.000000





- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

STANDARD SIGN W8 - 71WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 03/18/13

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\W871.DGN

1 5/8

2 1/4

2 1/4

2 1/4

3/4

3/4

3/4

2 1/2 8 1/2 8 3/8 9 3/4

4 1/4 10 1/4 10 1/8 11 3/4

HWY:

4 1/4 10 1/4 10 1/8 11 3/4 5

4 1/4 10 1/4 10 1/8 11 3/4 5

5

2M 36

4 48

5 48

PROJECT NO:

3 48

PLOT DATE: 18-MAR-2013 11:28

13 \\ 8 | 6 \\ 8 | 6 \\ 2 | 7 \\ 8

16 3/8 8 1/4 7 7/8 8 7/8

16 3/8 8 1/4 7 7/8 8 7/8

16 3/8 8 1/4 7 1/8 8 1/8

COUNTY:

PLOT NAME :

PLOT BY: mscsja

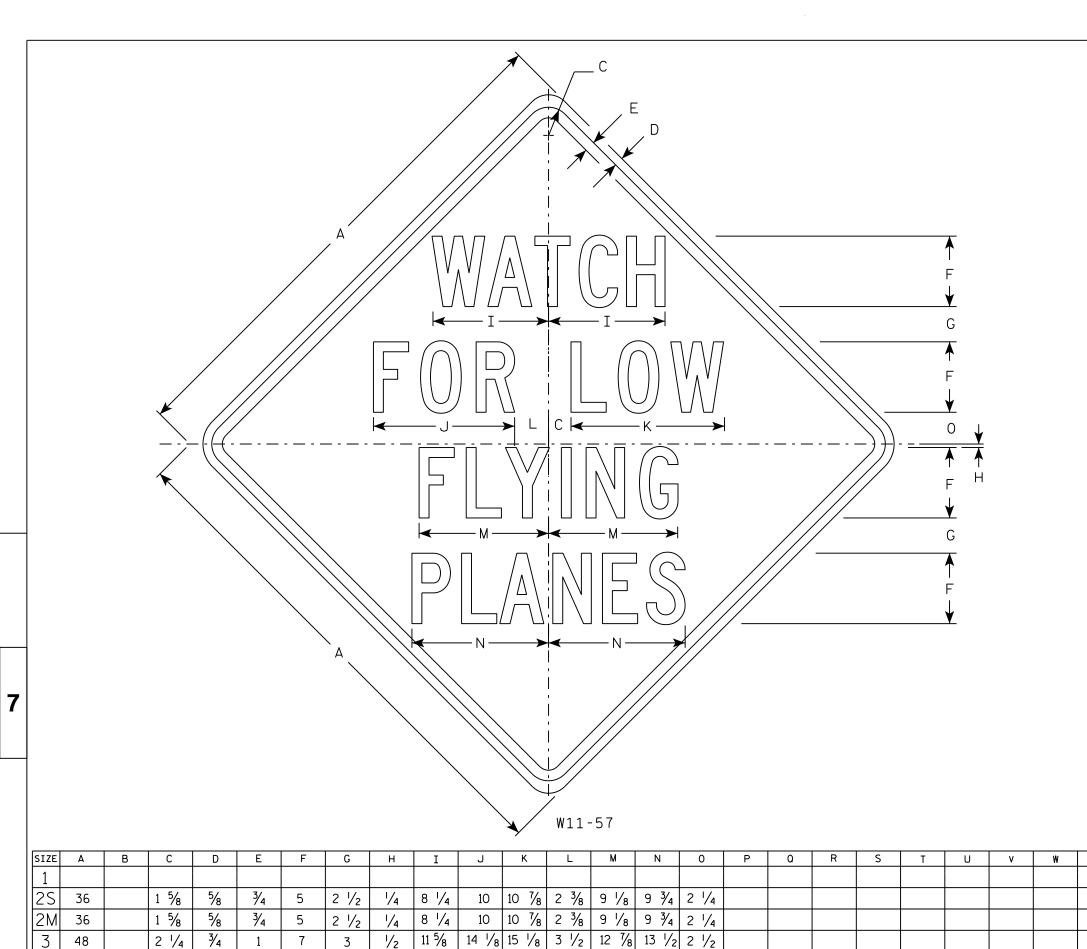
PLOT SCALE: 6.946654:1.000000

9.0

9.0

16

PLATE NO. W8-71.3



11 5/8 | 14 1/8 | 15 1/8 | 3 1/2 | 12 7/8 | 13 1/2 | 2 1/2

11 5/8 14 1/8 15 1/8 3 1/2 12 1/8 13 1/2 2 1/2

COUNTY:

1/2

HWY:

### NOTES

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

3. Message Series - C

9.0

9.0

16.0

16.0

16.0

W11-57

WISCONSIN DEPT OF TRANSPORTATION

STANDARD SIGN

APPROVED

For State Traffic Engineer

DATE 7/20/16

16 PLATE NO. W11-57.8

SHEET NO:

PROJECT NO:
FILE NAME • C•\CAFfiles\Projects\tr stdolate\W1157 DGN

2 1/4

2 1/4

48

48

PLOT DATE . 20-. || || -2016 10.40

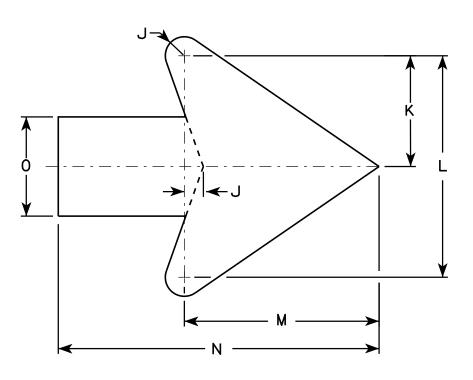
PINT RY . \$\$ plotuser \$\$ PINT NAMF :

PLOT SCALE . 8 810445.1 000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

SIZE	Α	В	С	D	E	F	G	н	I	J	K	L	M	N	0	Ρ	0	R	S	T	U	V	W	X	Y	Z	Areo
1																											
25	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
2M	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
3	30		1 3/8	1/2	5/8		10	5	11 %	3/4	4 1/2	9	7 1/8	13	4												6.25
4	36		1 3/8	1/2	5/8		12	6	14 1/4	1	5 ½	10 1/8	9 %	15 ¾	4 3/4												9.0
5	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0

COUNTY:

W12-1D

STANDARD SIGN W12-1D

WISCONSIN DEPT OF TRANSPORTATION

Fer State Traffic Engineer DATE 3/13/13 PLATE NO. W12-1D.15

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\W121D.DGN

PROJECT NO:

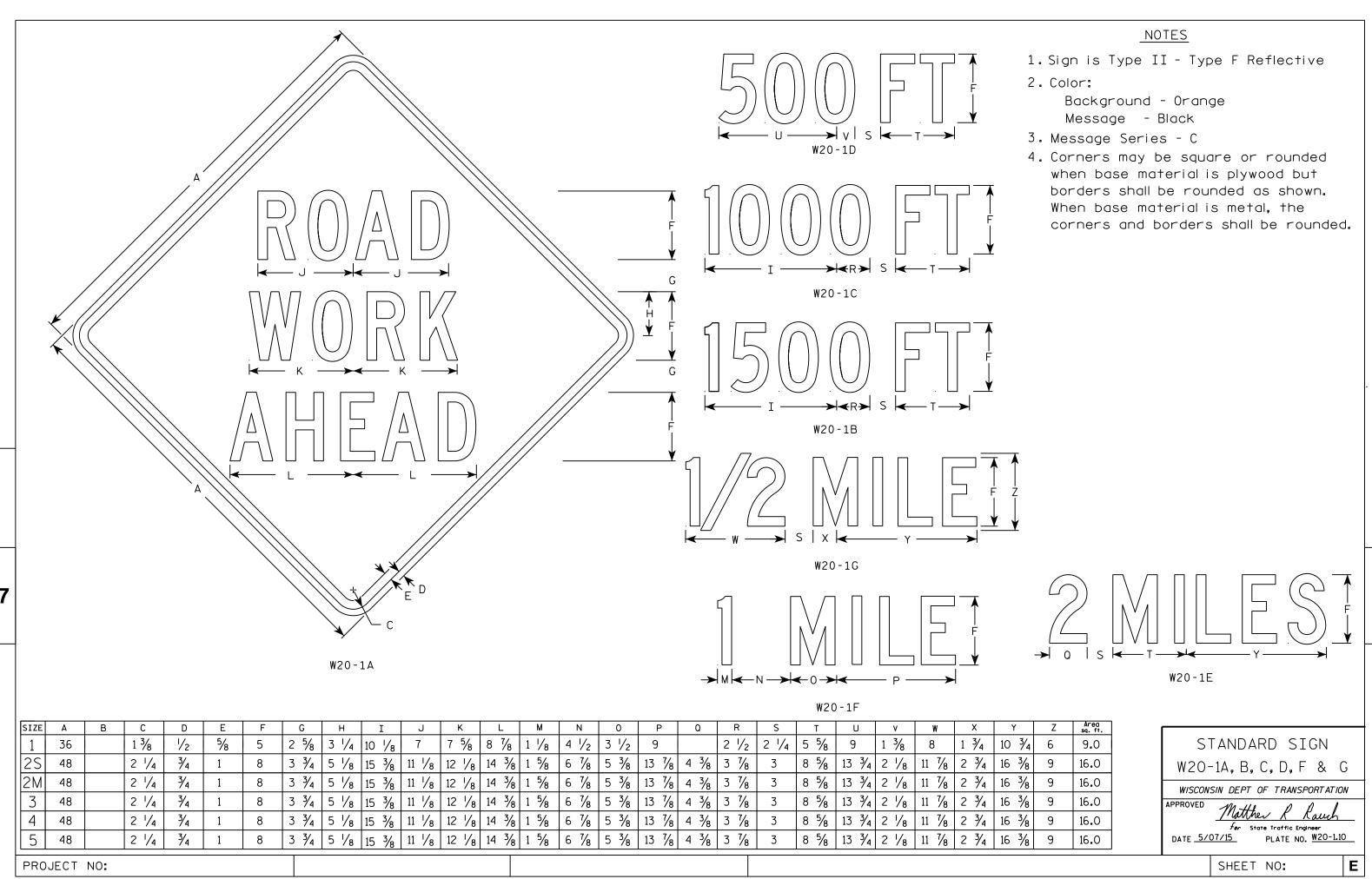
HWY:

PLOT DATE: 13-MAR-2013 13:26

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: 4.713802:1.000000



FILE NAME . C.\CAFfiles\Projects\tr stdolote\W201 DCN

PLOT DATE . 01-DEC-2015 18.24

PIOT RY \* \$\$ plotuser \$\$

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. " \_\_\_\_\_ LANE" is Series B. All other copy is Series C.

W20-5D

W20-5B

W20-5G

PLOT BY: mscj9h

->I0I← R-		
	W20-5F	

								W20-	5 A																	11 2	20-56
SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	v	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	<b>7/8</b>	2 1/2	13 1/8	10 ¾	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 1/8	5 %	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 1/8	7 1/2	13 1/2	3 3/8	2 3/8	10 %	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 5/8	7 1/2	13 1/2	3 %	2 3/8	10 %	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 %	1 1/2	6	4 %	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 %	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 %	1 1/2	6	4 5/8	12	2 %	7 1/2	13 1/2	3 ¾	2 3/8	10 %	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 %	7 1/2	13 1/2	3 <del>%</del>	2 3/8	10 %	16.0

COUNTY:

STANDARD SIGN W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Kauch Fer State Traffic Engineer DATE 3/18/11 PLATE NO. W20-5.11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr\_stdplate\W205.DGN

PROJECT NO:

HWY:

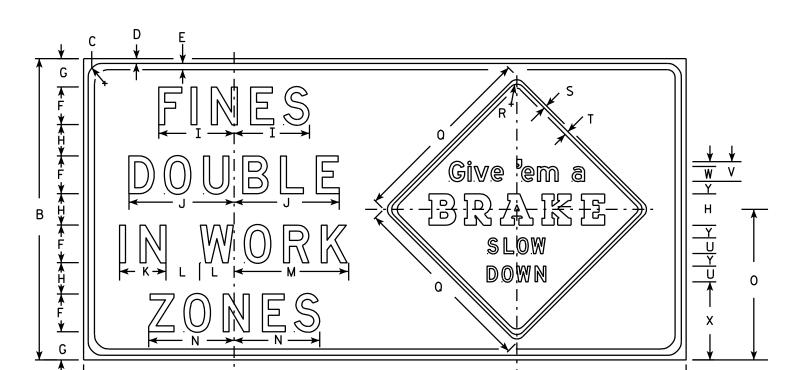
W20-56A

W20-55A

PLOT DATE: 18-MAR-2011 12:15

PLOT NAME :

PLOT SCALE: 11.918087:1.000000



W21-62

HWY:

### NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White - (See Note 5)
Message - Black

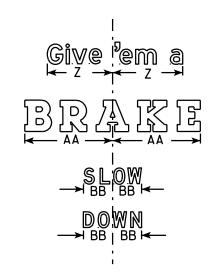
3. Message Series -

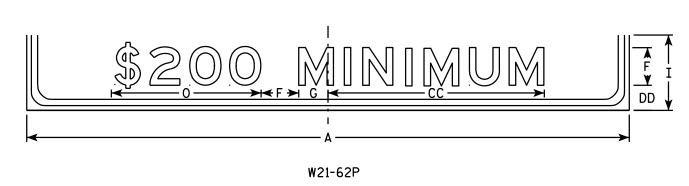
Fines Double Message - All lines are Series D Give 'em a Brake -

Line one is Series E, line two is a Special Graphic Series and lines three and four are Series D.

- 4. The base material shall be plywood. Corners may be square or rounded, but borders shall be rounded as shown. The base material for Give 'em a Brake sign can be a seperate sheet of aluminum with the corners and borders rounded as shown. This seperate panel shall then be attached to the plywood with aluminum or stainless steel sheet metal screws.
- 5. Background for the Give 'em a Brake sign shall be Type F reflective orange.

PLOT NAME :





SIZE 2S 2M 3 96 48 2 1/4 3/4 4 1/2 16 3/4 7 3/8 | 5 1/2 | 18 1/4 | 13 5/8 | 24 27 30 | 1 3/8 | 1/2 % | 2 ½ | 3 ½ | 2 ¾ | 12 ½ | 2 10 3/4 14 4 % 34 1/2 4 4 5

COUNTY:

STANDARD SIGN W21-62

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthe R Lawl

For State Traffic Engineer

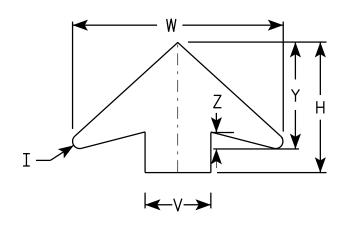
DATE 3/21/11 PLATE NO. W21-62.5

SHEET NO:

# <u>NOTES</u>

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color: \* Background - ORANGE\* Message - BLACK
- 3. Message Series C for numbers Series E for wording
- 4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

\*Speed Limit Sign shall have a White Background



ARROW DETAIL

SIZE	Α	В	С	D	Ε	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	C	٧	W	X	Y	Z	Areo sq. ft.
1	36		1 1/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3∕8	9 3/4	1 5/8	9.0
2S	48		2 1/4	3/4	1	19 1/4	10 ¾	17 3/8	1∕8	30	2 1/4	4	1 1/4	15	10	1 %	1/2	8	9 1/4	9 3%	12	8	25 %	3∕8	13	2	16.0
2M	48		2 1/4	3/4	1	19 1/4	10 ¾	17 3/8	7∕8	30	2 1/4	4	1 1/4	15	10	1 %	1/2	8	9 1/4	9 3/8	12	8	25 %	3⁄8	13	2	16.0
3	48		2 1/4	3∕4	1	19 1/4	10 ¾	17 3/8	7∕8	30	2 1/4	4	1 1/4	15	10	1 %	1/2	8	9 1/4	9 3%	12	8	25 %	3∕8	13	2	16.0
4	48		2 1/4	3/4	1	19 1/4	10 ¾	17 3/8	<b>⅓</b>	30	2 1/4	4	1 1/4	15	10	1 %	1/2	8	9 1/4	9 3%	12	8	25 %	3/8	13	2	16.0
5	48		2 1/4	3/4	1	19 1/4	10 ¾	17 3/8	<b>1</b> / <sub>8</sub>	30	2 1/4	4	1 1/4	15	10	1 %	1/2	8	9 1/4	9 3/8	12	8	25 %	3∕8	13	2	16.0

STANDARD SIGN W03 - 5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R Raul

DATE 11/20/13

PLATE NO. W03-5.1

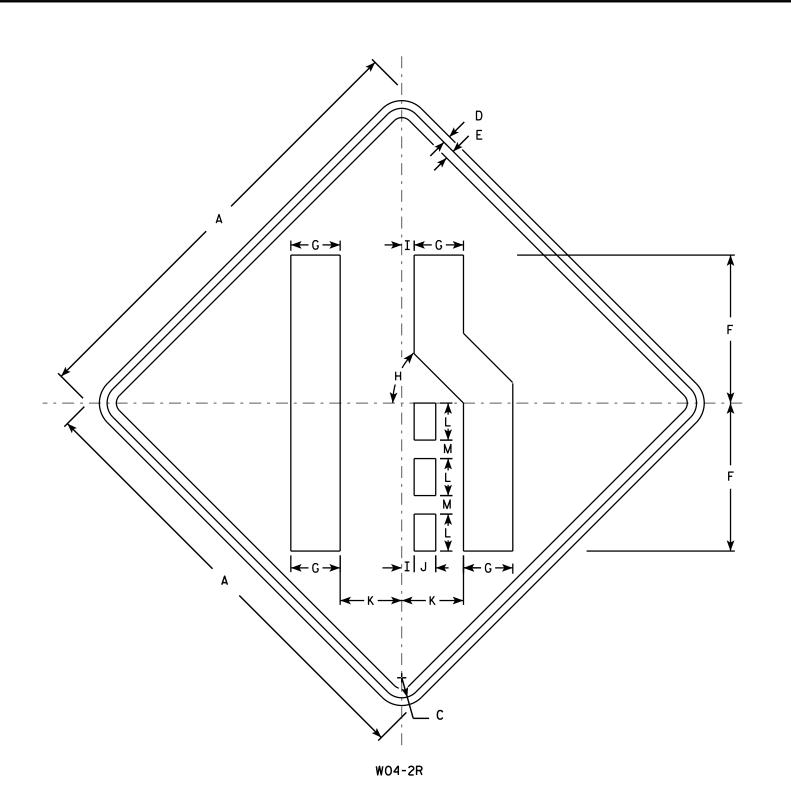
SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\W035.DGN

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W04-2L is the same as W04-2R except the symbolis reversed along the vertical centerline.



SIZE	Α	В	С	D	Ε	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Areg sq. ft.
1 2S 2M 3 4	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 ¾	4	2														16.0
2M	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 ¾	4	2														16.0
3	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 ¾	4	2														16.0
4	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 ¾	4	2														16.0
5	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 ¾	4	2														16.0

STANDARD SIGN WO4-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

//WMW/ / / Ad ForState Traffic Engineer

DATE 11/20/13 PLATE NO. W04-2.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr\_stdplate\W042.DGN

PROJECT NO:

PLOT DATE: 20-NOV-2013 11:43

PLOT BY: mscsja



# Wisconsin Department of Transportation

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