JUNE 2017

ORDER OF SHEETS

Title

Typical Sections and Details Estimate of Quantities

Miscellaneous Quantities

Standard Detail Drawings

Computer Earthwork Data

Right of Way Plat

Plan and Profile

Structure Plans

Sign Plates

Section No. 1

Section No. 2

Section No. 3

Section No. 5

Section No. 6

Section No. 7

Section No. 8

Section No. 9

TOTAL SHEETS = 110

DESIGN DESIGNATION

CONVENTIONAL SYMBOLS

LIMITED HIGHWAY FASEMENT

PROPOSED OR NEW R/W LINE

EXISTING RIGHT OF WAY

SLOPE INTERCEPT

REFERENCE LINE

EXISTING CULVERT

PROPOSED CULVERT

COMBUSTIBLE FLUIDS

WOODED OR SHRUB AREA

(Box or Pipe)

MARSH AREA

A.A.D.T.

A.A.D.T.

DESIGN SPEED

CORPORATE LIMITS

PROPERTY LINE

LOT LINE

D.H.V.

ESALS

D.D.

(2010) = 3,600

(2018) = 3,900

= 5.3

= 58/42

= 6.4%

= 60 mph

= 665,000

Section No. 9 Cross Sections

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

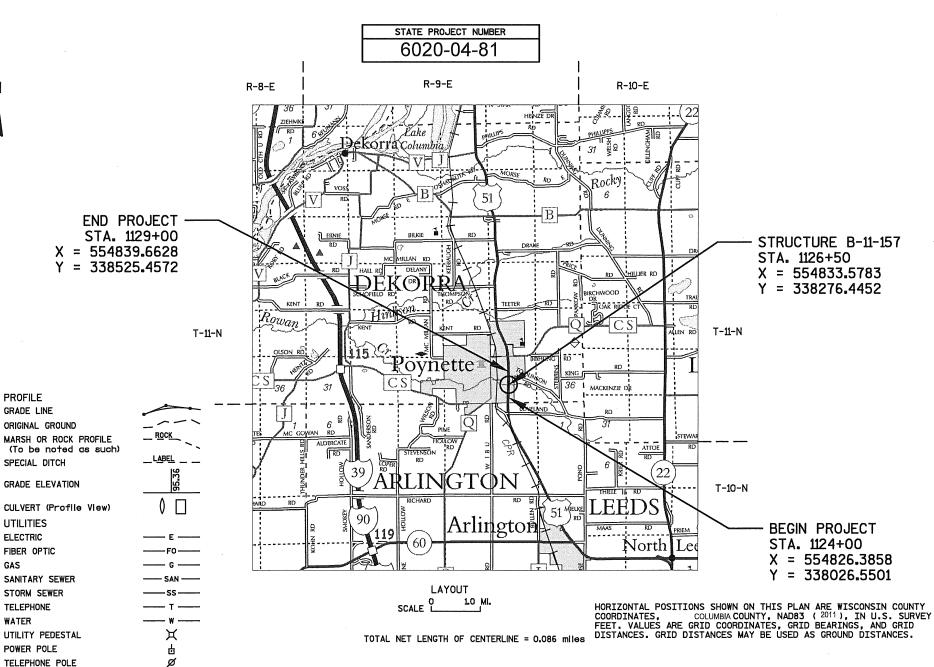
FEDERAL PROJECT STATE PROJECT PROJECT CONTRACT WISC 2017316 6020-04-81

PLAN OF PROPOSED IMPROVEMENT

DEFOREST - PORTAGE

ROWAN CREEK BRIDGE B-11-157

USH 51 COLUMBIA COUNTY



GAS

WATER

STATE OF WISCONSIN

DEPARTMENT OF TRANSPORTATION

ROBERT A TALARCZYK

EMRAN BHUIYAN

BOB LEX

REGIONAL EXAMINER

KURT JOHNSON

CO EXAMINER

PREPARED BY

C.O. Examiner

APPROVED FOR THE DEPARTMENT

Surveyor

2

NO TREES SHALL BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING, BIKE, OR PARKING LANE,

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/INCH.

PLACE THE 5" HMA PAVEMENT IN TWO LAYERS.

PRIOR TO PLACEMENT OF MGS GUARDRAIL, THE SHOULDERS SHALL BE IN PLACE, SHAPED AND COMPACTED.

EXISTING SHOULDER AGGREGATE SHALL BE INCORPORATED INTO THE NEW SHOULDERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER IN THE FIELD.

EXCESS MATERIAL ON THE EXISTING SHOULDERS SHALL BE SHAPED TO ALLOW A MINIMUM 2 1/2" DEPTH OF NEW CRUSHED AGGREGATE SHOULDERS.

EPOXY PAVEMENT MARKING SHALL CONSIST OF CENTERLINE AND EDGELINE MARKINGS ON THE FINAL SURFACE COURSE.

PURSUANT TO CHAPTER 59 OF THE WISCONSIN STATUTES, THE CONTRACTOR SHALL CAREFULLY SEARCH FOR EVIDENCE OF A LANDMARK IN ALL AREAS WHERE SUCH A LANDMARK MAY EXIST.

THE CONTRACTOR IS TO WORK WITH THE UTMOST CARE AND PROTECT ALL SURVEY MARKERS, REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.

THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER.

LIST OF STANDARD ABBREVIATIONS

ABUT AC AADT ASPH AVG BL BM CB CC/L CC CE CONC CTH CY C&G CULV CPAS DHV DIA DD E X ELEC ELEV ESALS EXC EBS	ABUTMENT ACRE ANNUAL AVERAGE DAILY TRAFFIC ASPHALT AVERAGE BASE LINE BENCH MARK CATCH BASIN CENTER LINE CENTER TO CENTER COMERCIAL ENTRANCE CONCRETE COUNTY COUNTY TRUNK HIGHWAY CUBIC YARD CURB AND GUTTER CULVERT CONCRETE PAVEMENT APPROACH SLAB DESIGN HOUR VOLUME DIAMETER DIRECTIONAL DISTRIBUTION EAST EAST GRD COORDINATE ELECTRIC ELEVATION EQUIVALENT SINGLE AXLE LOADS EXCAVATION EXCAVATION BELOW SUBGRADE EXISTING FIELD ENTRANCE FACE TO FACE FINISHED GRADE FLOW LINE FOOT HIGH EARLY STRENGTH HUNDREDWEIGHT HYDRANT INCH DIAMETER	INL ID INV IP JTT L L HF L S H MB MLC N Y NBO OD AVEN PET PCCS PE PCCS PE	INLET INSIDE DIAMETER INVERT IRON PIPE JOINT JUNCTION LEFT LENGTH OF CURVE LEFT HAND FORWARD LENEAR FOOT LITER LUMP SUM MANHOLE MESSAGE BOARD MAILBOX MATCH LINE NORMAL CROWN NORTH NORTH GRD COORDINATE NORTHBOUND NUMBER OUTSIDE DIAMETER PAVEMENT PERMANENT PERMANENT PERMANENT PORTLAND CEMENT CONCRETE PAVED CONCRETE PAVED CONCRETE PAVED CONCRETE PAVED CONCRETE PAVED CONCRETE PRIVATE ENTRANCE	RDWY SALV SSS SAN SEC SHLDR S SB SPECS SSF SST SDD STA SS STR SL TEMP TLE T C T YP UG	ROADWAY SALVAGED SANITARY AND STORM SEWER SANITARY SEWER SECTION SHOULDER SIDEWALK SOUTH SOUTHBOUND SPECIAL SPECIFICATIONS SQUARE SQUARE FEET SQUARE YARD STANDARD STANDARD DETAILS DRAWINGS STATE TRUNK HIGHWAYS STATION STORM SEWER STRUCTURE OR STRUCTURAL SURVEY LINE TELEPHONE TEMPORARY TEMPORARY TEMPORARY LIMITED EASEMENT TON TOP OF CURB TRUCKS (PERCENT OF) TYPICAL UNDERGROUND
DD F	DIRECTIONAL DISTRIBUTION FAST	OD PAVT	OUTSIDE DIAMETER PAVEMENT	TEL TEMP	TELEPHONE TEMPORARY
X	EAST GRD COORDINATE	PERM	PERMANENT	TLE	TEMPORARY LIMITED EASEMENT
FLEC FLEV	ELECTRIC FLEVATION	PLE PT	PERMANENT LIMITED EASEMENT POINT	TC	TOP OF CURB
ESALS	EQUIVALENT SINGLE AXLE LOADS	PCC	PORTLAND CEMENT CONCRETE	Ť	TRUCKS (PERCENT OF)
EXC	EXCAVATION BELOW SUBCRADE	PCS	PAVED CONCRETE SHOULDER	TYP	TYPICAL
EXIST	EXISTING	PROJ	PROJECT	USH	UNITED STATES HIGHWAY
FE	FIELD ENTRANCE	PL	PROPERTY LINE	VAR	VARIABLE
FF	FACE TO FACE	R	RADIUS	VERT	VERTICAL
FG FL	FINISHED GRADE	K/L PEOD	REFERENCE LINE	W	WATER WATER MAIN
FT	FOOT	RT	RIGHT	WV	WATER MAIN WATER VALVE
HES	HIGH EARLY STRENGTH	RHF	RIGHT HAND FORWARD	w	WEST
CWT	HUNDREDWEIGHT	R/W	RIGHT-OF-WAY	WB	WEST BOUND
HYD	HYDRANT	RD	ROAD	YD	YARD
IN DIA	INCH DIAMETER				

UTILITIES CONTACTS

TONY MARCINIAK
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LA CROSSE, WI 54602
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steve.blado@centurytel.com

DNR CONTACT

ERIC HEGGELUND DNR SOUTH CENTRAL REGION HQ 3911 FISH HATCHERY ROAD FITCHBURG, WI 53711 PH: (608)275-3301 eric.heggelund@wisconsin.gov



WISDOT CONTACT

ROBERT K LEX, P.E.
PROJECT MANAGER
PDS-SOUTH UNIT
SW REGION-MADIOSN OFFICE
2101 WRIGHT ST.
MADISON, WI 53704
PH: (608)246-5622
robert.lex@dot.wi.gov

EMRAN BHUIYAN, P.E. PROJECT ENGINEER PDS-SOUTH UNIT SW REGION-MADIOSN OFFICE 2101 WRIGHT ST. MADISON, WI 53704 PH: (608)246-7549 mohammad.bhuiyan@dot.wi.gov

PROJECT NO:6020-04-81

HWY:USH 51

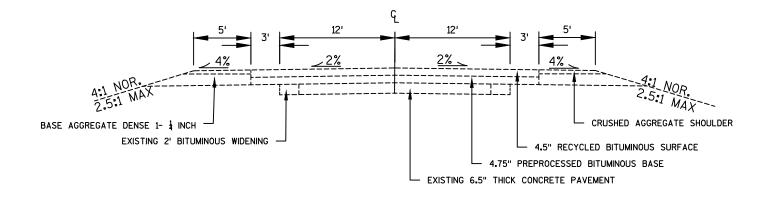
COUNTY: COLUMBIA

GENERAL NOTES

SHEET

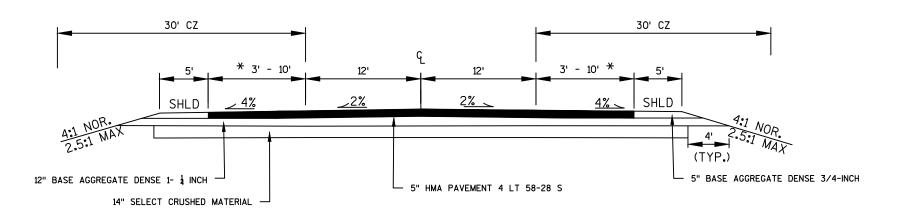
ΙE

FILE NAME : N:\PDS\C3D\60200401\SHEETSPLAN\020101_GN.DWG PLOT BY : BHUIYAN, MOHAMMAD E PLOT NAME : PLOT SCALE : 1 IN:100 FT



TYPICAL EXISTING SECTION

STA. 1124+00 TO STA. 1129+00



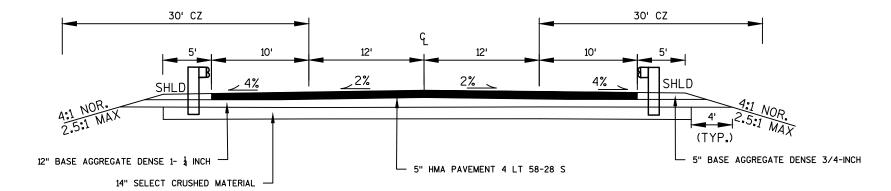
* 10' ASPHALT SHOULDER MATCH WITH 3' EXISTING SHOULDER STA. 1124+00 & STA. 1129+00

TYPICAL FINISHED SECTION

STA. 1124+00 TO STA. 1124+27 STA. 1128+55 TO STA. 1129+00 (WITHOUT GUARDRAIL)

PROJECT NO:6020-04-81 HWY: USH 51 COUNTY: COLUMBIA TYPICAL SECTION SHEET **E**



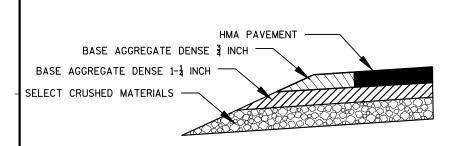


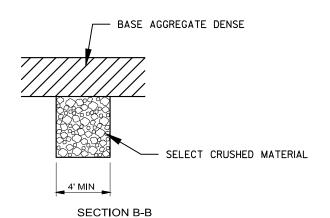
TYPICAL FINISHED SECTION

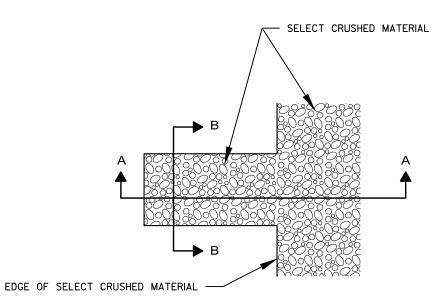
STA. 1124+27 TO STA. 1125+80.42 STA. 1126+99.58 TO STA. 1128+55 (WITH GUARDRAIL)

PROJECT NO:6020-04-81 HWY: USH 51 COUNTY: COLUMBIA TYPICAL SECTION SHEET **E**

FILE NAME : N:\PDS\C3D\60200401\SHEETSPLAN\020301_TS.DWG PLOT BY : BHUIYAN, MOHAMMAD E PLOT NAME : PLOT SCALE : 1 IN:10 FT WISDOT/CADDS SHEET 42





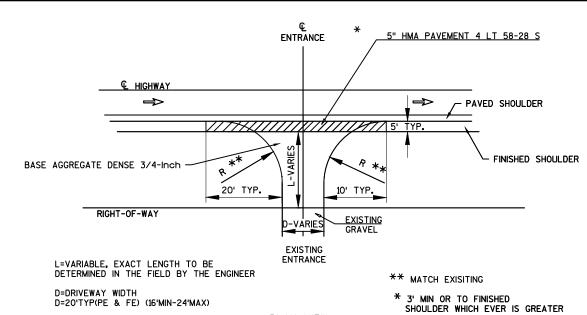


SECTION A-A

DETAIL FOR FRENCH DRAINS

EXCAVATION REQUIRED TO CONSTRUCT FRENCH DRAINS SHALL BE CONSIDERED INCIDENTAL TO THE ITEM SELECT CRUSHED MATERIAL.

> STA. 1125+50 LT & RT STA. 1127+25 LT & RT



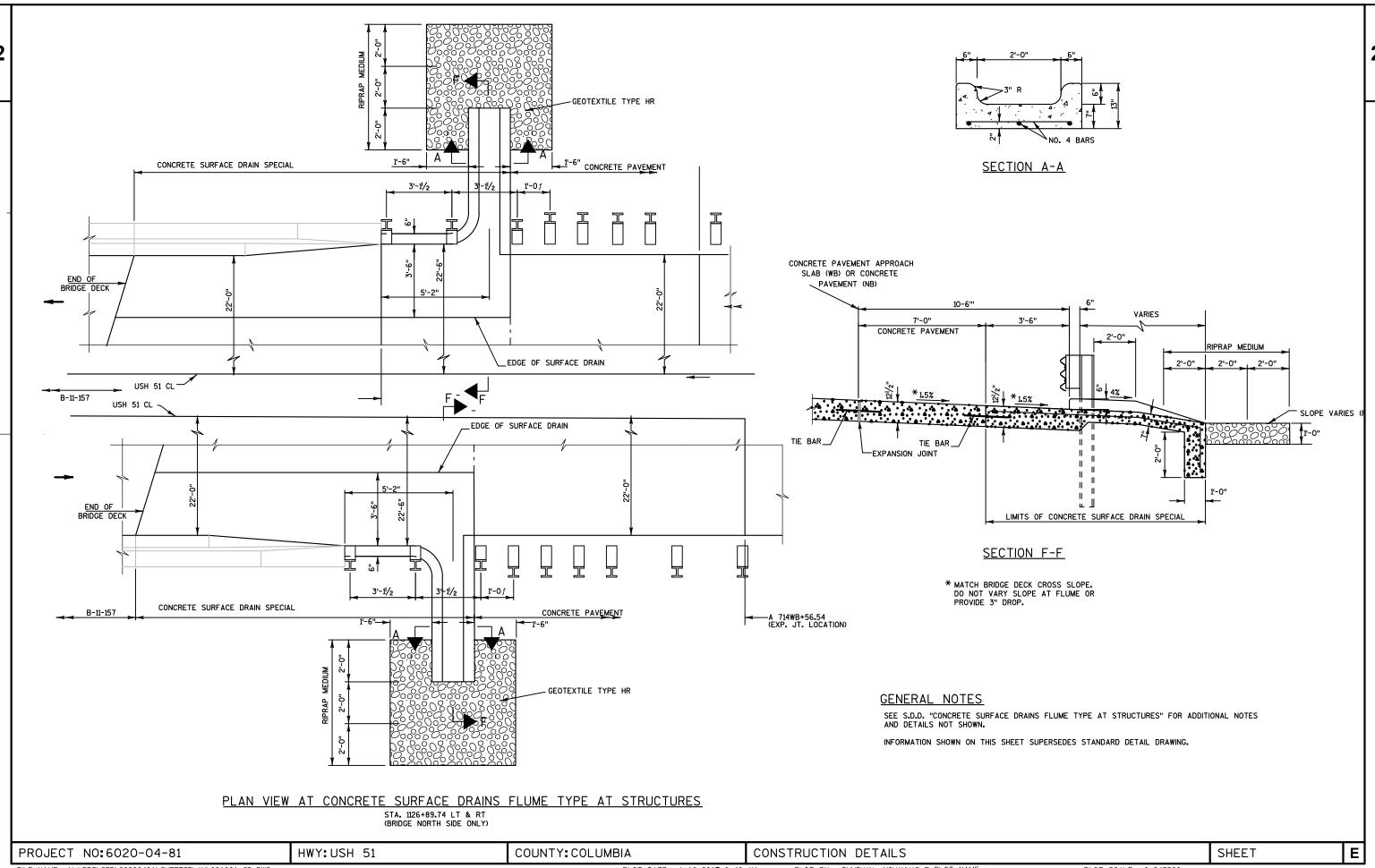
PLAN VIEW

FIELD ENTRANCES, RURAL DRIVEWAYS, AND TEE INTERSECTIONS (PE, FE & CE)

STA. 1128+10 RT (WDNR ENTRANCE)

HWY: USH 51 COUNTY: COLUMBIA SHEET Ε PROJECT NO:6020-04-81 CONSTRUCTION DETAILS

FILE NAME: N:\PDS\C3D\60200401\SHEETSPLAN\021001_CD.DWG PLOT DATE: 11/28/2016 2:57 PM PLOT BY : BHUIYAN, MOHAMMAD E PLOT NAME : PLOT SCALE : 1 IN:200 FT WISDOT/CADDS SHEET 42



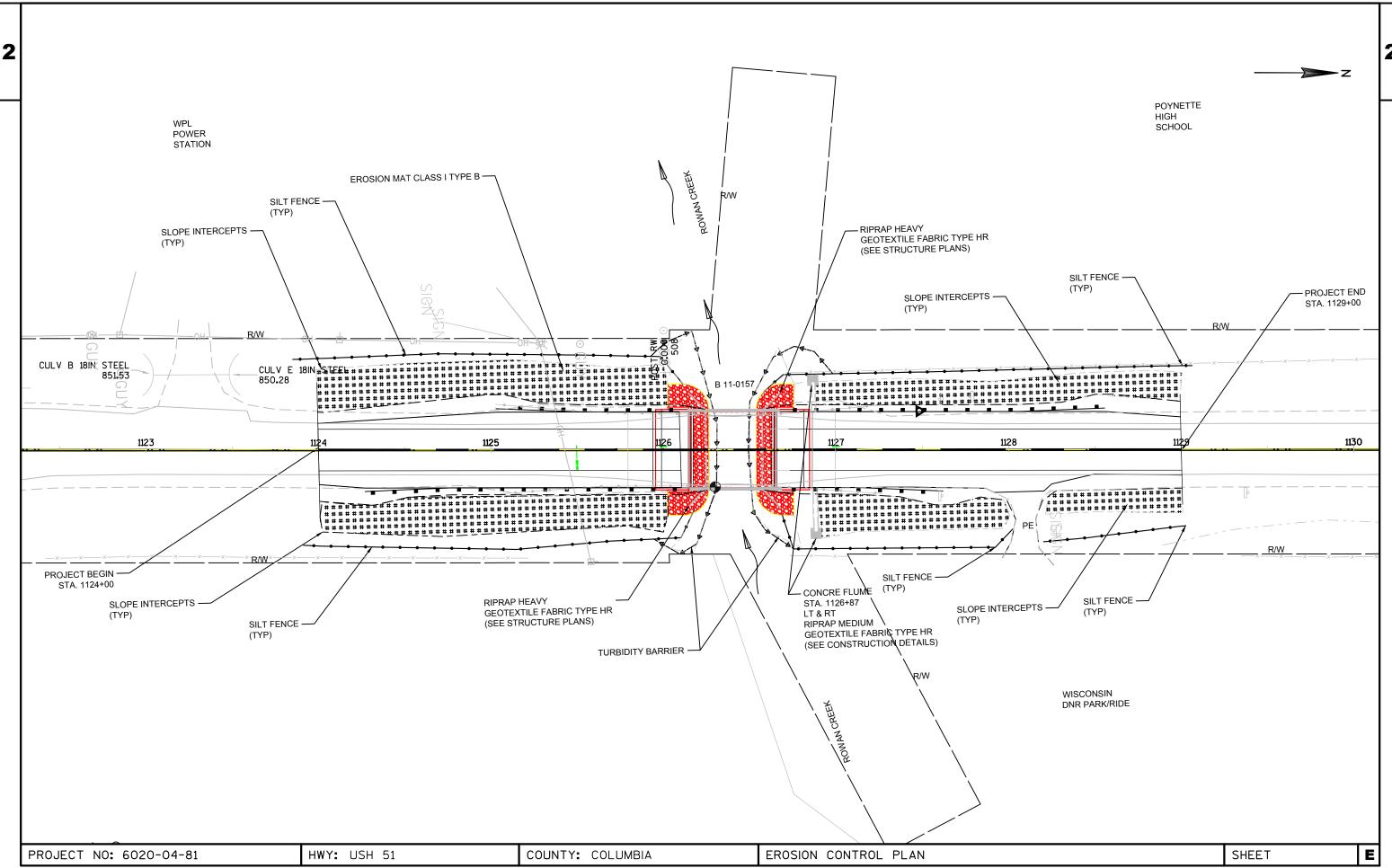
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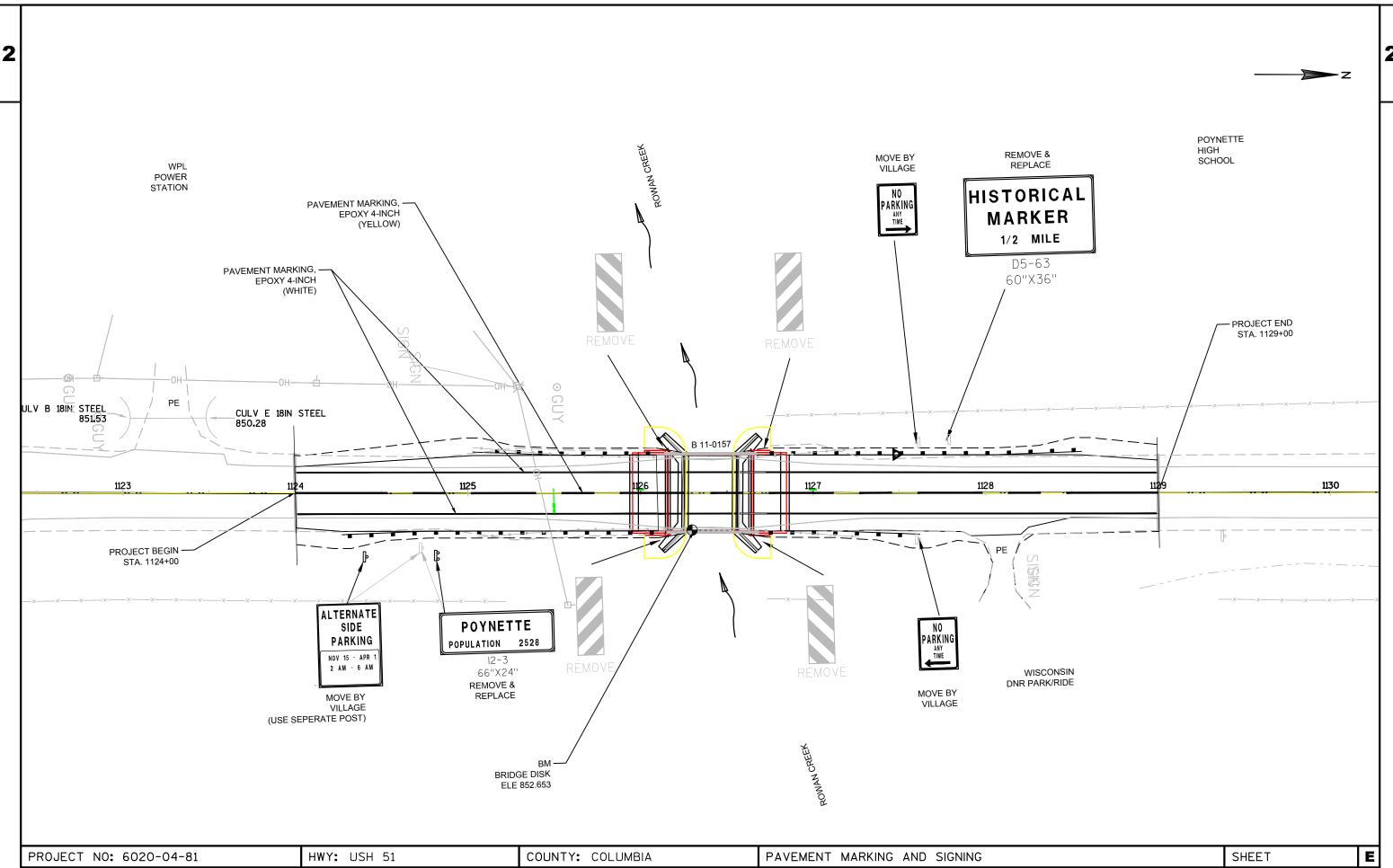
PLOT DATE: 1/19/2017 9:49 AM

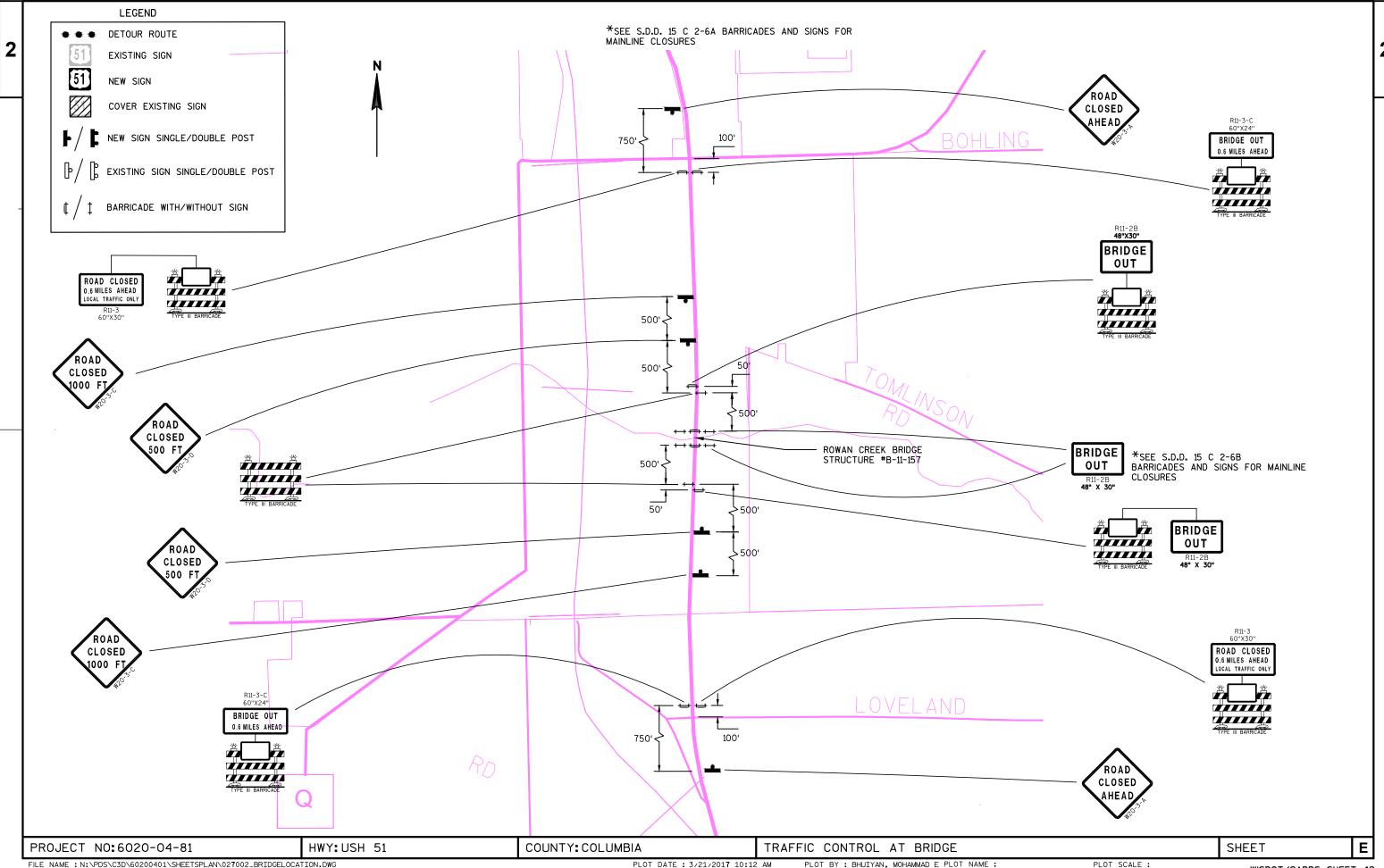
PLOT BY: BHUIYAN, MOHAMMAD E PLOT NAME:

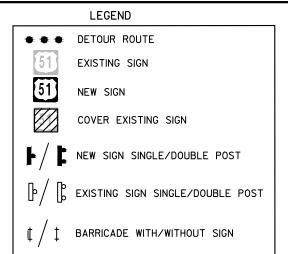
PLOT SCALE: 0.243560

WISDOT/CADDS SHEET 42

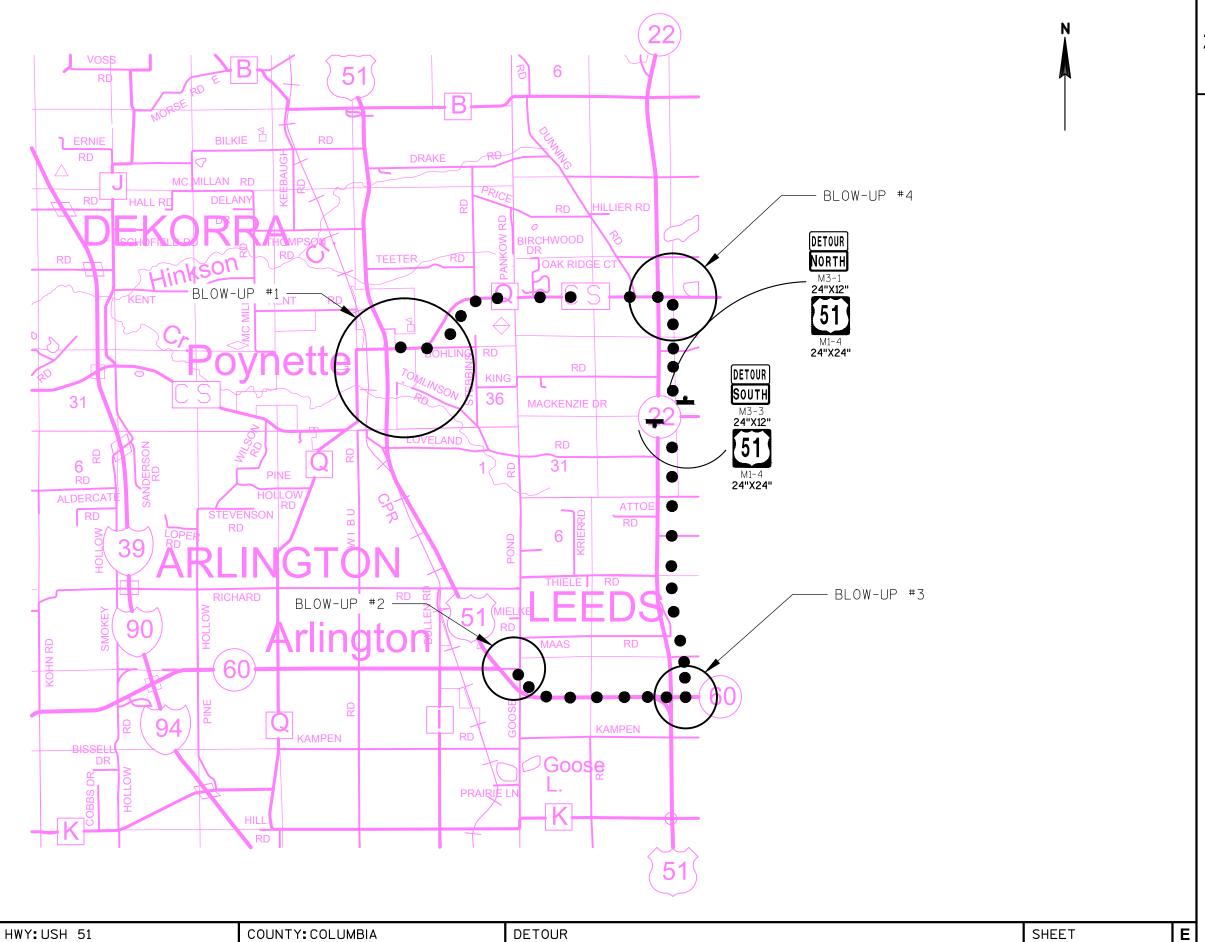








SIGNS SHOWN ON THIS PAGE ARE TO BE PAID FOR WITH THE ITEM TRAFFIC CONTROL DETOUR SIGNS UNLESS OTHERWISE INDICATED.



PROJECT NO:6020-04-81 HWY:USH 51 COUNTY:COLUMBIA DETOUR

FILE NAME:N:NPDS\C3D\60200401\SHEETSPLAN\027001_DT.DWG PLOT BY:BHUIYAN, MOHAMMAD E PLOT NAME: PLOT SCALE:

WISDOT/CADDS SHEET 42

COVER EXISTING SIGN

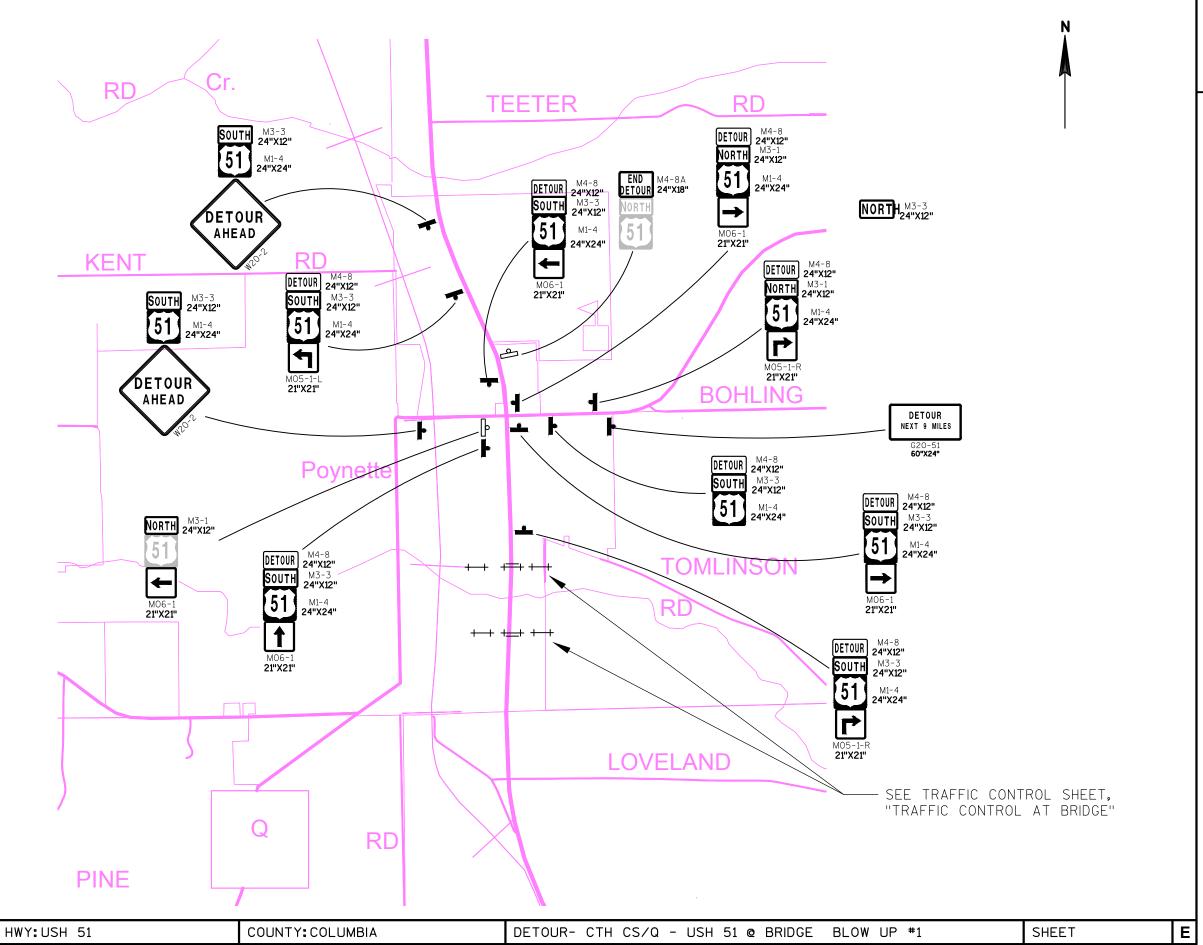
COVER EXISTING SIGN

NEW SIGN SINGLE/DOUBLE POST

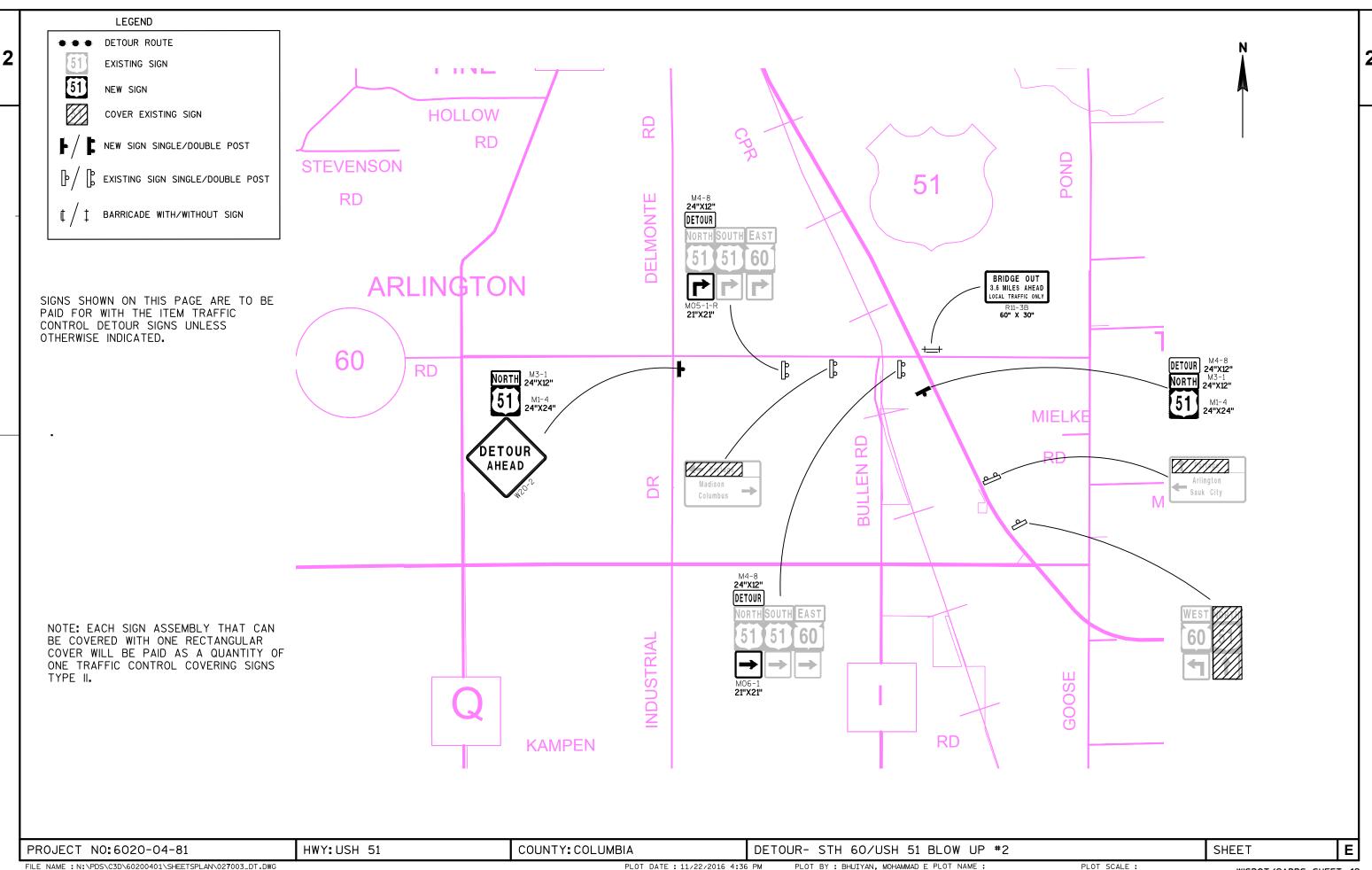
EXISTING SIGN SINGLE/DOUBLE POST

/ # BARRICADE WITH/WITHOUT SIGN

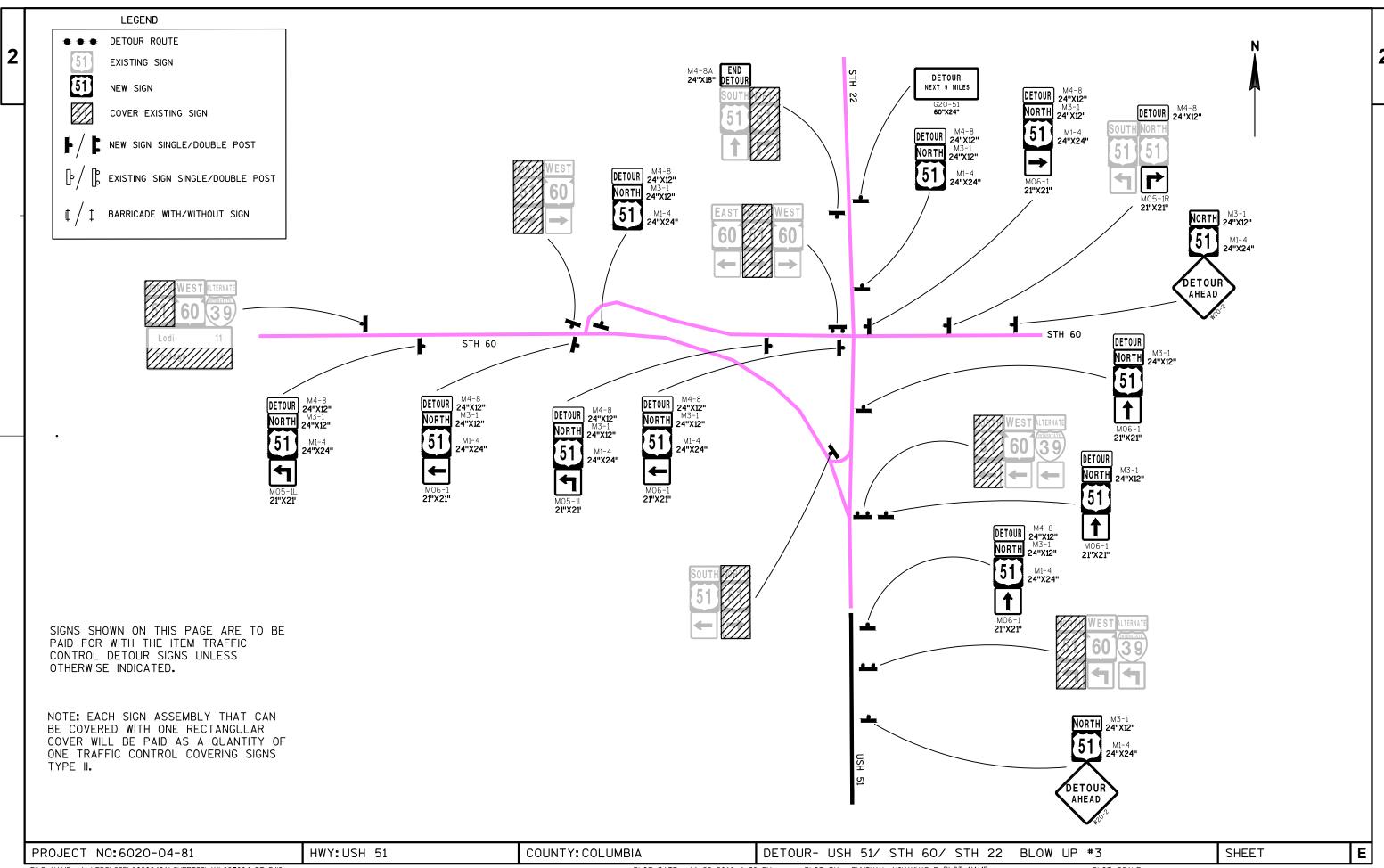
SIGNS SHOWN ON THIS PAGE ARE TO BE PAID FOR WITH THE ITEM TRAFFIC CONTROL DETOUR SIGNS UNLESS OTHERWISE INDICATED.



PROJECT NO:6020-04-81

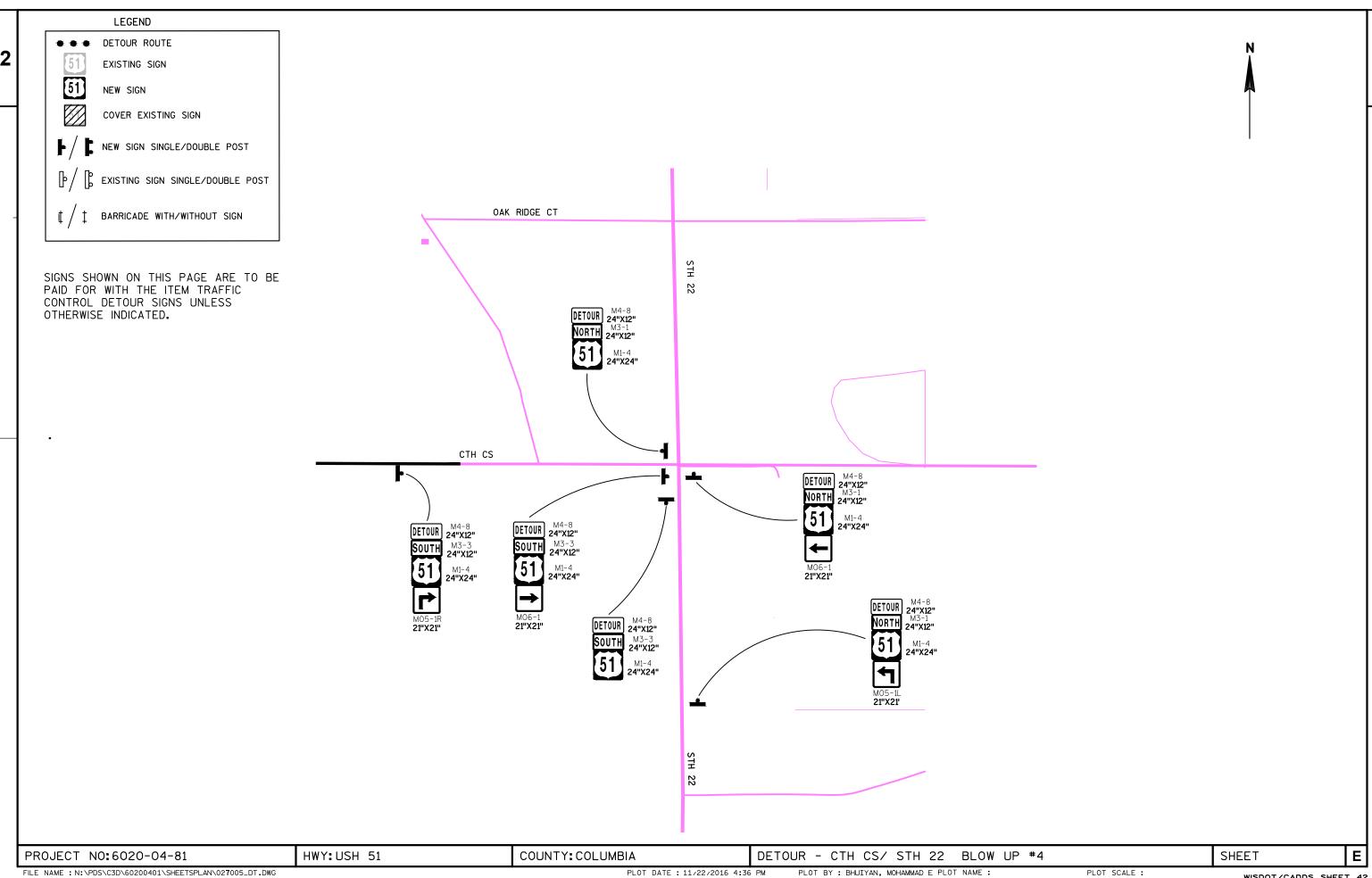


WISDOT/CADDS SHEET 42



FILE NAME: N:\PDS\C3D\60200401\SHEETSPLAN\027004_DT.DWG

PLOT DATE: 11/22/2016 4:36 PM
PLOT BY: BHUIYAN, MOHAMMAD E PLOT NAME:
PLOT SCALE:
WISDOT/CADDS SHEET 42



FILE NAME: N:\PDS\C3D\60200401\SHEETSPLAN\027005_DT.DWG

PLOT SCALE :

Estimate Of Quantities

ຂເ	າວດ	_∩⊿	_Q1

					6020-04-81	
Line	Item	Item Description	Unit	Total	Qty	
010	203.0700.S	Removing Old Structure Over Waterway With Debris Capture System (station) 01. 1126+24	LS	1.000	1.000	
20	204.0100	Removing Pavement	SY	1,703.000	1,703.000	
30	204.0110	Removing Asphaltic Surface	SY	1,521.000	1,521.000	
40	204.0165	Removing Guardrail	LF	400.000	400.000	
50	205.0100	Excavation Common	CY	3,078.000	3,078.000	
60	206.1000	Excavation for Structures Bridges (structure) 01. B-11-157	LS	1.000	1.000	
70	210.1500	Backfill Structure Type A	TON	201.000	201.000	
30	213.0100	Finishing Roadway (project) 01. 6020-04-81	EACH	1.000	1.000	
90	305.0110	Base Aggregate Dense 3/4-Inch	TON	285.000	285.000	
00	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2,434.000	2,434.000	
10	312.0110	Select Crushed Material	TON	2,515.000	2,515.000	
20	415.0120	Concrete Pavement 12-Inch	SY	72.000	72.000	
30	415.0410	Concrete Pavement Approach Slab	SY	90.000	90.000	
40	455.0605	Tack Coat	GAL	260.000	260.000	
50	460.2000	Incentive Density HMA Pavement	DOL	1,200.000	1,200.000	
60	460.5224	HMA Pavement 4 LT 58-28 S	TON	700.000	700.000	
70	502.0100	Concrete Masonry Bridges	CY	428.000	428.000	
30	502.3200	Protective Surface Treatment	SY	461.000	461.000	
0	502.3210	Pigmented Surface Sealer	SY	75.000	75.000	
00	505.0400	Bar Steel Reinforcement HS Structures	LB	6,110.000	6,110.000	
10	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	59,320.000	59,320.000	
20	505.0800.S	Bar Steel Reinforcement HS Stainless Structures	LB	440.000	440.000	
30	516.0500	Rubberized Membrane Waterproofing	SY	24.000	24.000	
0	550.0500	Pile Points	EACH	20.000	20.000	
0	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	600.000	600.000	
60	606.0200	Riprap Medium	CY	3.000	3.000	
70	606.0300	Riprap Heavy	CY	205.000	205.000	
30	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	185.000	185.000	
90	614.0150	Anchor Assemblies for Steel Plate Beam Guard	EACH	4.000	4.000	
00	614.2300	MGS Guardrail 3	LF	180.000	180.000	
10	614.2500	MGS Thrie Beam Transition	LF	160.000	160.000	
20	614.2610	MGS Guardrail Terminal EAT	EACH	4.000	4.000	
30	618.0100	Maintenance And Repair of Haul Roads (project) 01. 6020-04-81	EACH	1.000	1.000	
10	619.1000	Mobilization	EACH	1.000	1.000	
50	624.0100	Water	MGAL	25.000	25.000	
60	625.0500	Salvaged Topsoil	SY	2,910.000	2,910.000	
70	628.1504	Silt Fence	LF	1,220.000	1,220.000	

					6020-04-81
Line	Item	Item Description	Unit	Total	Qty
0380	628.1520	Silt Fence Maintenance	LF	1,220.000	1,220.000
0390	628.1905	Mobilizations Erosion Control	EACH	5.000	5.000
0400	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0410	628.2004	Erosion Mat Class I Type B	SY	2,785.000	2,785.000
0420	628.6005	Turbidity Barriers	SY	190.000	190.000
0430	629.0210	Fertilizer Type B	CWT	4.000	4.000
0440	630.0130	Seeding Mixture No. 30	LB	92.000	92.000
0450	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	4.000	4.000
0460	637.2210	Signs Type II Reflective H	SF	15.000	15.000
0470	638.2602	Removing Signs Type II	EACH	6.000	6.000
0480	638.3000	Removing Small Sign Supports	EACH	7.000	7.000
0490	643.0100	Traffic Control (project) 01. 6020-04-81	EACH	1.000	1.000
0500	643.0420	Traffic Control Barricades Type III	DAY	2,130.000	2,130.000
0510	643.0705	Traffic Control Warning Lights Type A	DAY	4,260.000	4,260.000
0520	643.0900	Traffic Control Signs	DAY	1,278.000	1,278.000
0530	643.0920	Traffic Control Covering Signs Type II	EACH	10.000	10.000
0540	643.2000	Traffic Control Detour (project) 01. 6020-04-81	EACH	1.000	1.000
0550	643.3000	Traffic Control Detour Signs	DAY	10,650.000	10,650.000
0560	645.0120	Geotextile Type HR	SY	353.000	353.000
0570	646.0106	Pavement Marking Epoxy 4-Inch	LF	1,775.000	1,775.000
0580	650.4500	Construction Staking Subgrade	LF	1,000.000	1,000.000
0590	650.5000	Construction Staking Base	LF	1,000.000	1,000.000
0600	650.9910	Construction Staking Supplemental Control (project) 01. 6020-04-81	LS	1.000	1.000
0610	650.9920	Construction Staking Slope Stakes	LF	1,100.000	1,100.000
0620	690.0150	Sawing Asphalt	LF	100.000	100.000
0630	715.0415	Incentive Strength Concrete Pavement	DOL	500.000	500.000
0640	715.0502	Incentive Strength Concrete Structures	DOL	2,568.000	2,568.000
0650	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	250.000	250.000
0660	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	500.000	500.000
0670	SPV.0035	Special 01. Concrete Surface Drain Special	CY	5.000	5.000

EARTHWORK SUMMARY

Mass 205.0100 Salvaged/Unusable Ordinate **Pavement Material** Reduced EBS Expanded Fill +/-**Common Excavation** in Division From/To Station Location (1) (4) Fill (9) (13) (14) Comment: Cut (2) EBS Excavation (3) Factor **Factor** 0.80 1.25 1124+00 - 1126+16 USH 51 2,931 147 117 758 2,174 1126+64 - 1129+00 2,931 147 117 758 2,174 Subtotal 0

Notes:

Grand Total

(1) Common Excavation is the sum of the Cut and EBS Excavation columns. Item number 205.0100 is in CY.

3,078

147

- (2) Salvaged/Unsuable Pavement Material is included in Cut.
- (3) EBS Excavation to be backfilled with Select Borrow material. Note: this is designers choice, can be backfilled with Borrow, or Cut as well.
- (4) Salvaged/Unusable Pavement Material

Total Common Exc

5) Available Material = Cut - Salvaged/Unusuable Pavement Material

2,931

- (9) Reduced EBS in Fill Excavated EBS material is usuable in Fills outside the 2:1 slope. EBS in Fill Reduction factor = 0.8
- (11) Expanded EBS Backfill This is to be filled with suitable useable material as directed by the engineer.
- (13) Expanded Fill Factor = 1.25
- (14) The Mass Ordinate + or Qty calculated for the Division. Plus quantity indicates an excess of material within the Division. Minus indicates a shortage of material within the Division.

0

117

758

2,174

PROJECT NO: 6020-04-81 HWY: USH 51 COUNTY: COLUMBIA MISCELLANEOUS QUANTITIES SHEET: **E**

FILE NAME: N:\PDS\...\030200_mq.pptx PLOT DATE: June 14, 1911 PLOT BY: A.R.H. PLOT NAME: PLOT SCALE: 1:1

		REMOVING PAVEMENT			
			(204. 0100) REMOVI NG PAVEMENT		
3	CATEG 001		(SY) REMARKS 744 SOUTH OF BRIDGE 884 NORTH OF BRIDGE 75 1703		
		REMOVING ASPHALTIC SUR	FACE		
		(204. (REMOV ASPHA SURF	VI NG LTI C		
	CATEGORY 0010	STATI ON - STATI ON LOCATI ON (SY 1124+00 - 1125+80 US 51 78 1127+00 1129+00 - 66 UNDI STRI BUTED 70 PROJECT TOTAL: 152	Y) REMARKS 66 SOUTH OF BRI DGE 65 NORTH OF BRI DGE 0		
		TROSECT TOTAL.			
		REMOVING GUA	RDRAI L		
			(204. 0165) REMOVI NG GUARDRAI L		
		CATEGORY STATION - STATION LOCATION 0010 1125+00 - 1126+25 US 51 II 1126+57 - 1127+32 US 51 II 1126+57 - 1127+32 US 51 II 1126+57 - 1127+32 US 51 II	RT 125 SOUTH OF BRIDGE LT 125 RT 75 NORTH OF BRIDGE		
		PROJECT TOTA			
PROJECT NO: 6020-04-81	HWY: USH 51	COUNTY: COLUMBIA	MISCELLANEOUS QUANTIT	IES	SHEET: E

BASE AGGREGATES

					(305. 0110)	(305. 0120)	(312. 0110)	
					BASE	BASE	SELECT	
					AGGREGATE	AGGREGATE	CRUSHED	
					DENSE 3/4-INCH	DENSE 1 1/4-INCH	MATERI AL	
CATEGORY	STATI ON	-	STATI ON	LOCATI ON	(TON)	(TON)	(TON)	REMARKS
0010	1124+00	-	1126+25	RT & LT	110	1020	1020	SOUTH OF BRIDGE
	1126+57	-	1129+00	RT & LT	150	1030	1120	NORTH OF BRIDGE
				EBS	-	-	265	EBS-COMMON
0020				BRI DGE	-	294	-	BRIDGE ITEM
				UNDI STRI BUTED	25	90	110	EXCAVATI ON
			PROJECT TOTAL:		285	2434	2515	

CONCRETE PAVEMENT 12-INCH

				(415. 0120) CONCRETE PAVEMENT	(SPV. 0035. 001) CONCRETE SURFACE DRAIN	(645. 0120) GEOTEXTI LE TYPE	
				12- I NCH	SPECI AL	HR	
CATEGORY	STATION -	STATI ON	LOCATI ON	(SY)	(CY)	(SY)	REMARKS
0010	1126+10 -	1126+25	BEFORE BRIDGE	36	-	-	PAVED CONCRETE SHOULDER
	1126+57 -	1126+72	AFTER BRIDGE	36	-	-	PAVED CONCRETE SHOULDER
	1126+90 -	-	LT & RT	-	5	8	SURFACE DRAIN
	PROJECT TOTAL:				5	8	

PROJECT NO: 6020-04-81 HWY: USH 51 COUNTY: COLUMBIA MISCELLANEOUS QUANTITIES SHEET: **E**

CONCRETE PAVEMENT APPROACH SLAB

(415. 0410)
CONCRETE
PAVEMENT
APPROACH SLAB

CATEGORY	STATION -	STATI ON	LOCATI ON	(SY)	REMARKS
0010	1125+80 -	1125+95	CNTR	45	REINFORCED CONCRETE SLAB
	1126+85 -	1127+00	CNTR	45	REINFORCED CONCRETE SLAB

PROJECT TOTAL: 90

HMA PAVEMENT

(460. 5224) (455.0605)HMA TACK PAVEMENT COAT 4 LT 58-28 S CATEGORY STATION -STATI ON LOCATI ON (GAL) (TON) REMARKS 0010 LT & RT 126 SOUTH OF BRIDGE 1124+00 1126+10 330 1126+72 -1129+00 LT & RT 134 370 NORTH OF BRIDGE

PROJECT TOTAL:

BEAM GUARD

260

700

					(614. 2300) MGS GUARDRAI L 3	(614. 2500) MGS THRI E BEAM TRANSI TI ON	(614. 2610) MGS GUARDRAI L TERMI NAL EAT	
CATEGORY	STATI ON	-	STATI ON	LOCATI ON	(LF)	(LF)	(EACH)	REMARKS
				STH 69				
0010	1124+22	-	1126+10	RT	90	40	1	SOUTH OF BRIDGE
	1125+02	-	1126+10	LT	-	40	1	
	1126+74	-	1127+62	RT	-	40	1	NORTH OF BRIDGE
	1126+74	-	1128+54	LT	90	40	1	
			PROJI	ECT TOTAL:	180	160	4	

PROJECT NO: 6020-04-81 HWY: USH 51 COUNTY: COLUMBIA MISCELLANEOUS QUANTITIES SHEET: **E**

WATER

(624.01	(00)
---------	------

					WATER	
CATEGORY	STATI ON	TO	STATI ON	LOCATI ON	(MGL)	REMARKS
				USH 51		
0010	1123+00	-	1126+50	LT & RT	16	AGGREGATES
	1126+50	-	1129+50	LT & RT	10	
		P	ROJECT TOTAL:		25	

LANDSCAPI NG

CATEGORY	STATI ON	ТО	STATI ON	LOCATI ON	(625. 0500) SALVAGED TOPSOI L (SY)	(629. 0210) FERTI LI ZER TYPE B (CWT)	(630. 0130) SEEDI NG MI XTURE NO. 30 (LB)	REMARKS
0010				USH 51				
	1124+00	-	1126+00	LT	970	1	30	
	1123+00	-	1126+00	RT	710	1	24	
	1126+50	-	1129+50	LT	550	1	18	
	1126+50	-	1129+00	RT	680	1	20	
				PROJECT TOTAL:	2910	4	92	

EROSION CONTROL

				(606. 0200)	(628. 1504)	(628. 1520)	(628. 1905) MOBI LI ZATI ONS	(628. 1910) MOBI LI ZATI ONS	(628. 2004) EROSI ON MAT	(628. 6005) TURBI DI TY	
				RI PRAP	SILT	SILT FENCE	EROSI ON	EMERGENCY	CLASS 1	BARRI ER	
				MEDI UM	FENCE	MAI NTENANCE	CONTROL	EROSI ON CONTROL	TYPE B		
CATEGORY STATION	T0	STATI ON	LOCATI ON	(CY)	(LF)	(LF)	(EACH)	(EACH)	(SY)	(SY)	REMARKS
0010			USH 51								
1126+50	-	-	CREEK BOTH SIDE	-	-	-	1	-	-	190	BRI DGE
1126+90	-	-	LT & RT	3	-	-	-	-	-	-	CONCRETE SURFACE
1124+00	-	1126+00	LT	-	286	286	1	1	950	-	DRAIN FLUME
1124+00	-	1126+00	RT	-	331	331	1	-	700	-	
1126+50	-	1129+00	LT	-	320	320	1	-	510	-	
1126+50	-	1129+00	RT	-	283	283	1	1	625	=	
			PROJECT TOTAL:	3	1220	1220	5	2	2785	190	

	PROJECT NO: 6020-04-81	HWY: USH 51	COUNTY: COLUMBIA	MISCELLANEOUS QUANTITIES	SHEET:	E	
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TRAFFIC CONTROL

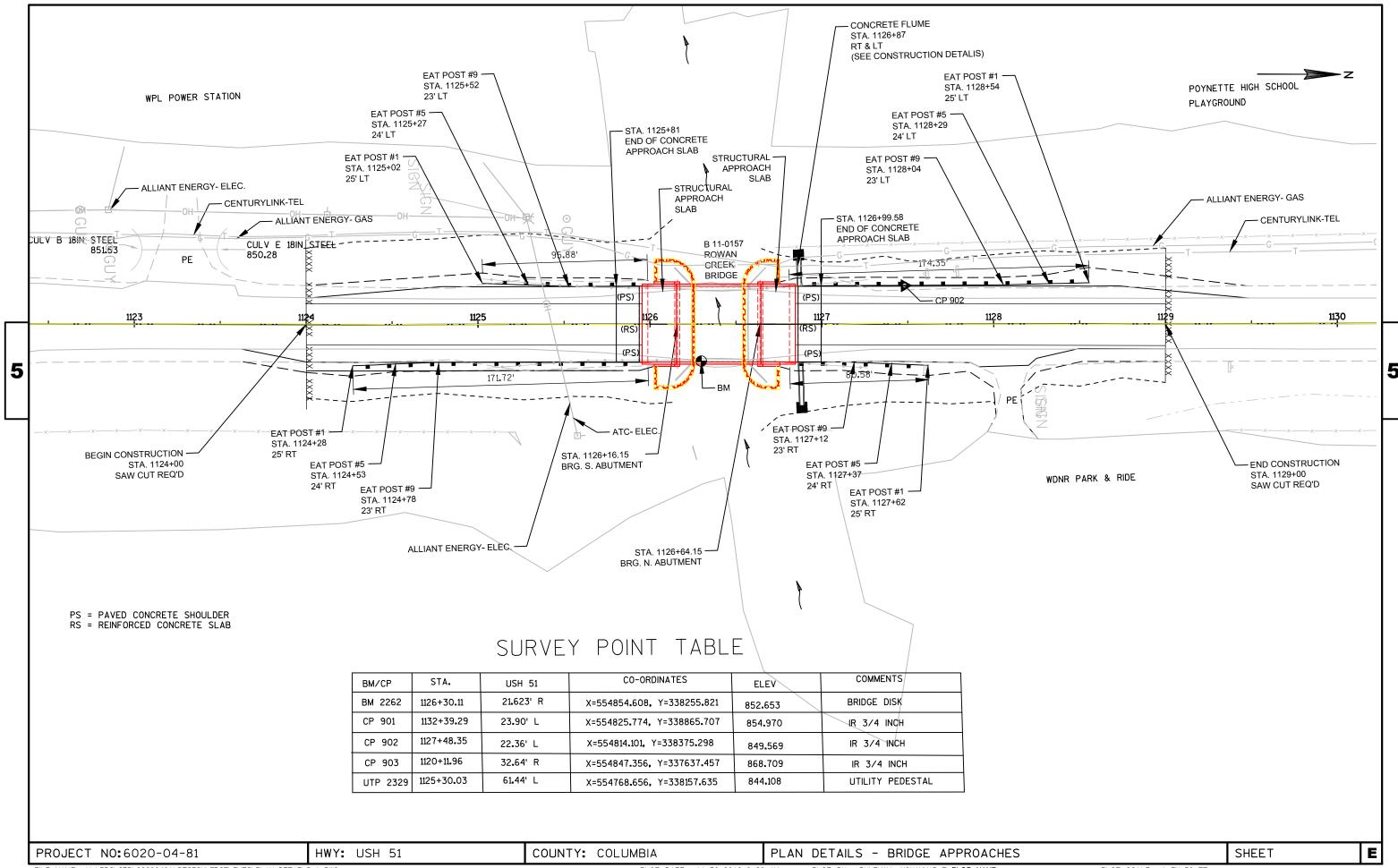
			PROJECT TOTAL:	2130	4260	1278	10		10650	
										NO STAGING
	DETOUR ROUTE	10	-	-	-	-	10	1	-	SIGN COVERING
	DETOUR ROUTE	150	71	-	-	-	-	-	10650	DETOUR
	ROWAN CRREK BRIDGE	12	71	852	1704	-	-	-	-	BARRI CADE
	ROWAN CRREK BRIDGE	18	71	1278	2556	-	-	-	-	BARRI CADE
	ROWAN CRREK BRIDGE	18	71	-	-	1278	-	-	-	TRAFFI C CONTROL
0010	USH 51									
CATEGORY	LOCATI ON	QUANTI TY PER DAY*		TYPE III (DAYS)	TYPE A (DAYS)	(DAYS)	TYPE II (EACH)	OF CYCLE	SI GNS (DAYS)	REMARKS
				BARRI CADES	WARNING LIGHTS	SI GNS	COVERING SIGNS	NUMBER	DETOUR	
				CONTROL	CONTROL	CONTROL	CONTROL		CONTROL	
				TRAFFI C	TRAFFI C	TRAFFI C	TRAFFI C		TRAFFI C	
				(643. 0420)	(643. 0705)	(643. 0900)	(643. 0920)		(643. 3000)	

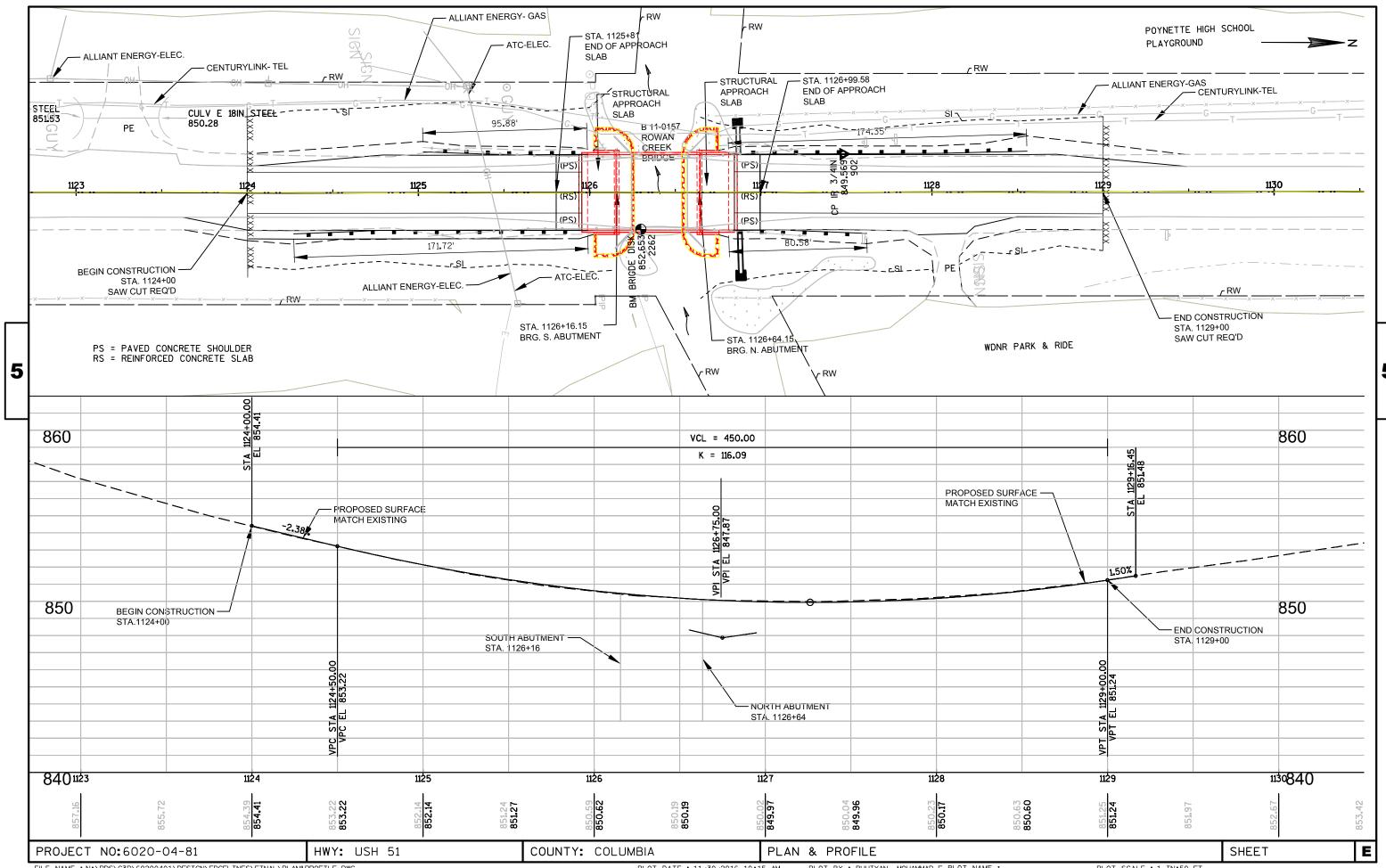
CONSTRUCTION STAKING

					(650. 4500) CONSTRUCTI ON STAKI NG SUBGRADE	(650. 5000) CONSTRUCTI ON STAKI NG BASE	(650. 9910) CONSTRUCTI ON STAKI NG SUPPLI MENTAL CONTROL (6020-04-81)	(650. 9920) CONSTRUCTI ON STAKI NG SLOPE STAKES	
CATEGORY	STATI ON	TO	STATI ON	LOCATI ON	(LF)	(LF)	(LS)	(LF)	REMARKS
0010	1124+00	-	1129+00	USH 51 -	1000	1000	1	1100	
1				PROJECT TOTAL:	1000	1000	1	1100	

PROJECT NO: 6020-04-81 HWY: USH 51 COUNTY: COLUMBIA MISCELLANEOUS QUANTITIES SHEET: **E**

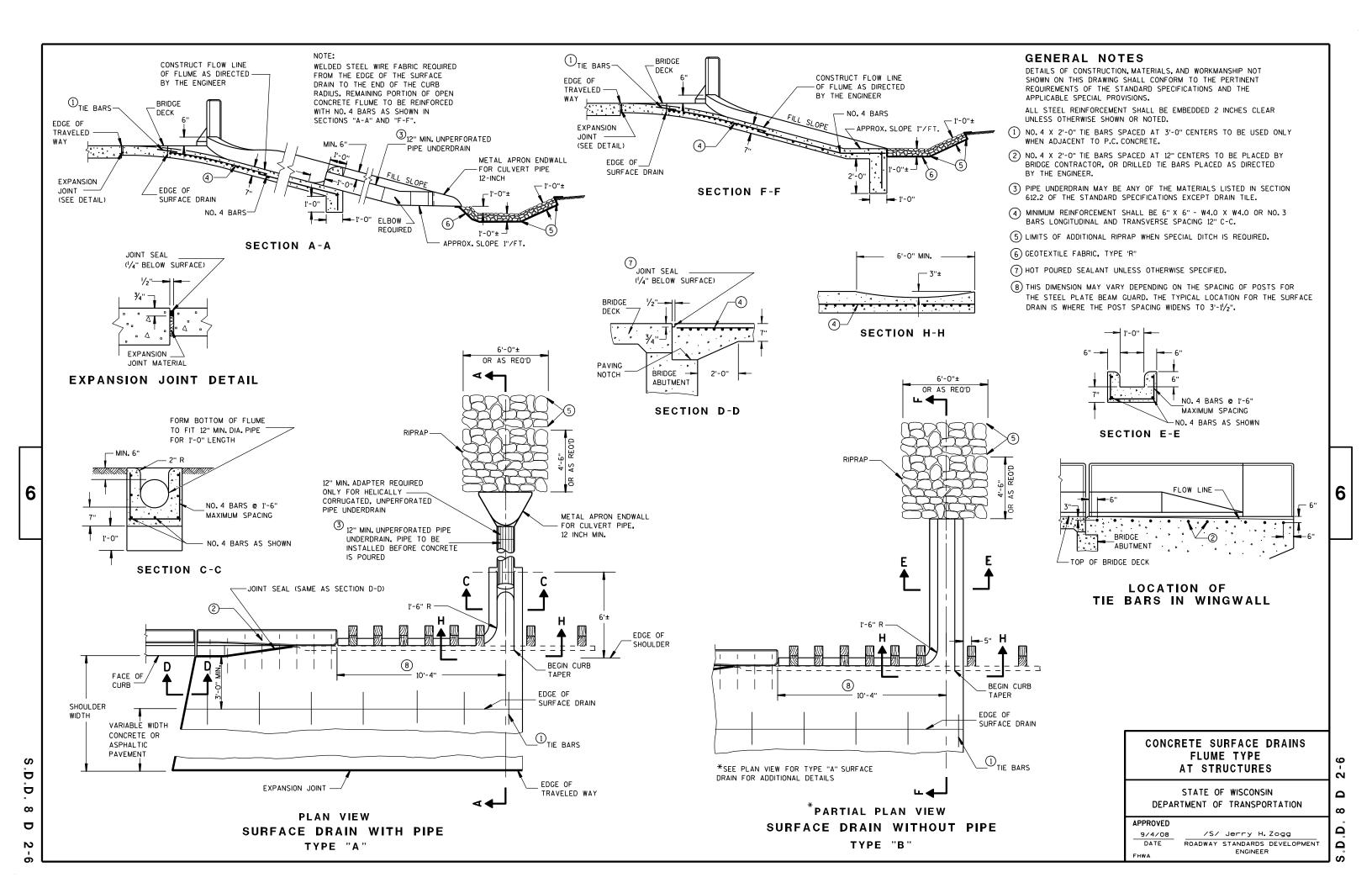
							SIG	NI NG							
								GN	SIZE	(634. 0616) POSTS WOOD 4X6-INCH X 16-FT	(637. 2210) SIGN TYPE II REFLECTIVE H	(638. 2602) REMOVING SIGNS TYPE II	(638, 3000) REMOVI NG SMALL SI GN SUPPORTS		
-	CATEGORY	SIGN CODE	STATI ON	T0	STATI ON		MES	SAGE		(EACH)	(SF)	(EACH)	(EACH)	REMARKS	-
	0010		1126+00	_	1127+00	USH 51 LT & RT	BRI DG	E SIGN	-	_	-	4	4		
		I 2-3	1124+75	-	-			ION SIGN	60" X 24"	2	10	1	2		
		D5-63	1127+75	-	-	LT I	HI STORI C	AL MARKER	60"X 36"	2	15	1	1		
-					PRO	JECT TOTAL:				4	15	6	7		-
								<u>PAVEMEN</u>	T MARKIN	NG_					
											(646. 0106) PAVEMENT MARKI NG EPOXY 4- I NCH				
					CATEGORY	STATI ON	то	STATI ON	LOCAT	YELLOW (LF)	WHI TI		REMARKS		
				-	CATEGURY	STATION	10	STATI ON	USH		(LF)		REMARKS		
					0010	1123+00	-	1129+50	LT RT		650				
						1123+00 1123+00	-	1129+50 1129+50	CENT		650				
				-			UN	NDI STRI BUTE	ED	100	200				
										275	1500	,			
									PROJECT	TOTAL:	1775				
								SAWI NG	ASPHALT						
											(690. 0150) SAWI NG ASPHALT				
				_(CATEGORY	STATI ON	T	O STA	ATI ON	LOCATI ON USH 51	(LF)	REMA	ARKS		
					0010	1124+00 1129+00			- -	TRANSVERSE TRANSVERSE	40 40	SOUTH OF NORTH OF			
				_						UNDI STRI BUTED PROJECT TOTAL:	20 100				
										INOSECI IVIAL:	100				
ROJECT NO: 6020-	L∩1-91		HWY	/·	 J 51		COL	NTY: COLU	IMRIA	MISOT	LLANEOUS QUANT	ITIEQ			SHEET:





Standard Detail Drawing List

08D02-06	CONCRETE SURFACE DRAINS FLUME TYPE AT STRUCTURES
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E11-02	TURBI DI TY BARRI ER
12A03-10	NAME PLATE (STRUCTURES)
13A03-06	CONCRETE PAVEMENT SHOULDERS
13B02-08A	CONCRETE PAVEMENT APPROACH SLAB
13B02-08B	STRUCTURAL APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB
13C01-18	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C18-04A	CONCRETE PAVEMENT JOINTING
13C18-04B	CONCRETE PAVEMENT STEEL REINFORCEMENT
13C18-04C	CONCRETE PAVEMENT JOINT TYPES
14B42-04A	MI DWEST GUARDRAI L SYSTEM (MGS) GUARDRAI L
14B42-04B	MI DWEST GUARDRAI L SYSTEM (MGS) GUARDRAI L
14B42-04C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-02A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B45-04A	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04B	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04C	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04D	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04E	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04F	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04G	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04H	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04I	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04J	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04K	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04L	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-03	BARRI CADES AND SIGNS FOR SIDEROAD CLOSURES
15C06-08	SIGNING & MARKING FOR TWO LANE BRIDGES
15C08-17A	LONGITUDINAL MARKING (MAINLINE)
	TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M.P.H. WITH BARRIER
	TRAFFIC CONTROL, LANE CLOSURE
15D12-06B	TRAFFIC CONTROL, LANE CLOSURE, SPEED REDUCTION



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF **EROSION BALES / TEMPORARY** DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 /S/ Beth Connestro
CHIEF ROADWAY DEVELOPMENT ENGINEER

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TYPICAL APPLICATION OF SILT FENCE

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PLAN VIEW SILT FENCE AT MEDIAN SURFACE DRAINS



GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- \bigcirc HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- 3 WOOD POSTS SHALL BE A MINIMUM SIZE OF 11/8" X 11/8" OF OAK OR HICKORY.
- 4) SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- (5) CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Cannestra

29-05 /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER

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D.D. 8 E 9

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GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- 2 SANDBAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- (3) WHEN BARRIER HEIGHT, H. EXCEEDS 8 FT., POST SPACING MAY NEED TO BE DECREASED.
- (4) IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- (5) ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MIMIMUM BARRIER HEIGHT SHALL BE 2'GREATER THAN EITHER THE 02 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WICHEVER IS GREATER.
- (6) FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BED ROCK PREVENTS THE INSTALLATION OF POSTS.
- (7) ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- (8) USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.





SECTION C-C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

TURBIDITY BARRIER

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 /S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER ∞

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TYPICAL NAME PLATE

(BRIDGES, CULVERTS, AND RETAINING WALLS)



NUMBERING DESIGNATION MULTI-UNIT STRUCTURES

GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- 1 EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- (2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SPREAD OPEN SO THE TOP OF LUG IS 11/4" WIDE

SECTION A-A

ALTERNATE LUG



ALTERNATE LUG

(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

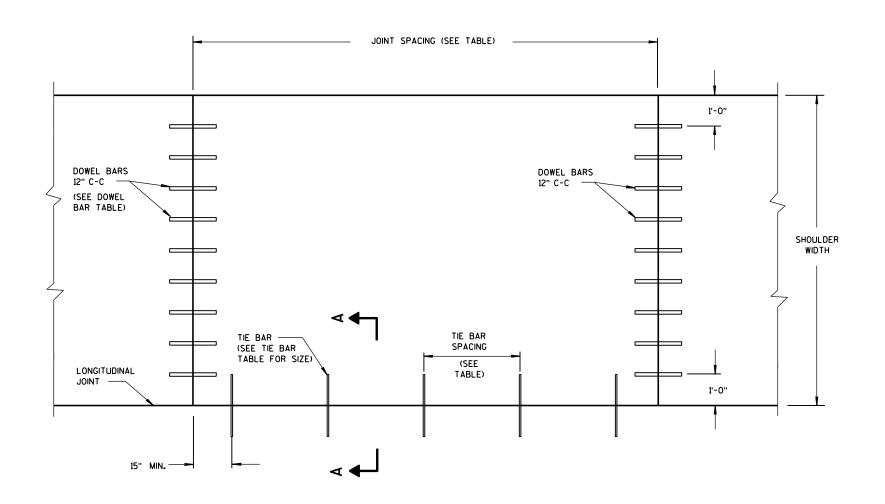
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3/26/IO /S/ SCOT BECKET

CHIEF STRUCTURAL DEVELOPMENT ENGINEER

D.D. 12 A

3-10



PLAN VIEW CONCRETE PAVEMENT SHOULDER

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR Size	TIE BAR LENGTH (L)	MAX. TIE BAR Spacing
< 10 1/2"	NO. 4	30"	36"
≥ 10 ½"	NO. 5	36"	36"
2 10 72	NO. 4 *	30"	24"**

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINUMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

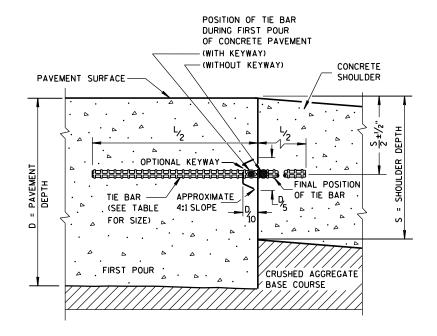
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TRANSVERSE JOINT DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.

FINISH THE SHOULDER PAVEMENT CONFORMING TO SUBSECTION 415.3.8 OF THE STANDARD SPECIFICATIONS.

TIE BARS SHALL CONFORM TO SUBSECTION 505.2.4 OF THE STANDARD SPECIFICATIONS.



SECTION A-A LONGITUDINAL CONSTRUCTION JOINT

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER***	CONTRACTION JOINT SPACING
5 ½", 6", 6 ½"	NONE	12'
7", 7 ½"	1"	14'
8", 8 ½"	1 1/4"	15'
9", 9 ½"	1 1/4"	15'
10" & ABOVE	11/2"	15'

FOR DOWELED CONCRETE SHOULDERS WITH TRAPEZOIDAL CROSS SECTIONS, CHOSE THE APPROPRIATE DOWEL BAR DIAMETER BASED ON THE SMALLER PAVEMENT DEPTH (LIKELY THE OUTSIDE EDGE OF THE SHOULDER). IF USING BASKETS, USE BASKETS FOR THE AVERAGE THICKNESS OF THE CROSS SECTION.

CONCRETE	PAVEMENT	SHOULDERS

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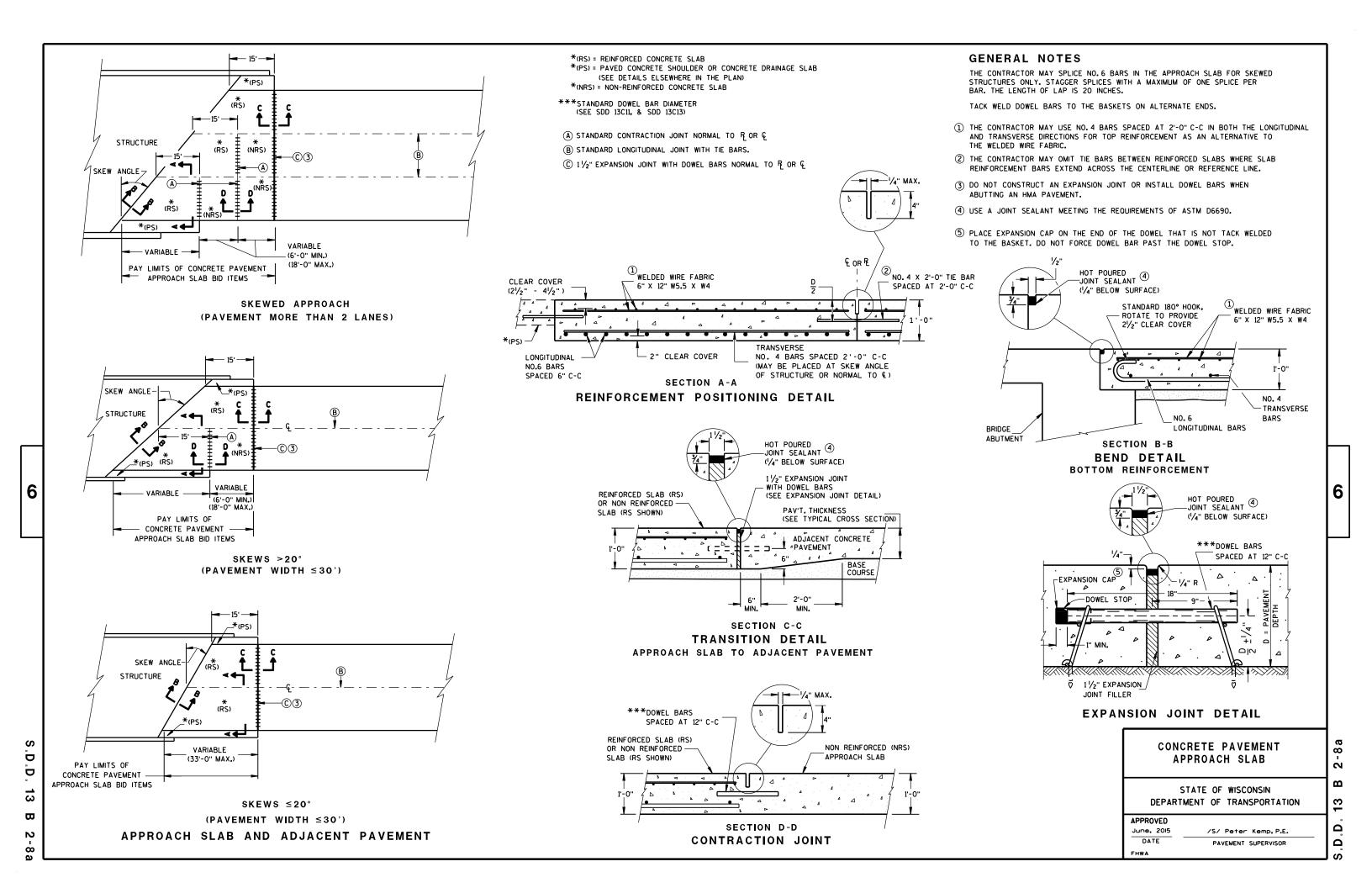
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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED	
June, 2015	/S/ Peter Kemp, P.E.
DATE	PAVEMENT SUPERVISOR

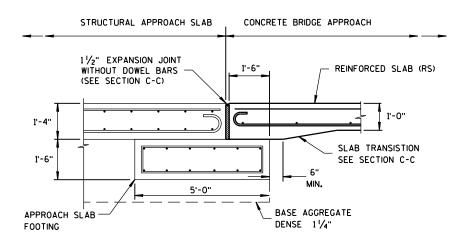


GENERAL NOTES

ALL PROJECTS THAT INVOLVE A STRUCTURAL APPROACH SLAB WILL ALSO HAVE A CONCRETE PAVEMENT APPROACH SLAB.

- 1 SEE BRIDGE PLAN.
- (2) CONFORM TO SHEET 13 B 2(A) FOR CONCRETE PAVEMENT APPROACH SLAB DETAILS.
- 3 DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.
- © 11/2" EXPANSION JOINT WITH DOWEL BARS NORMAL TO P OR &
- D 1 1/2" EXPANSION JOINT (NO DOWELS)

BRIDGE APPROACHES



SECTION E-E

FOOTING DETAIL

STRUCTURAL APPROACH SLAB TO CONCRETE BRIDGE APPROACH

STRUCTURAL APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED	
June, 2015	/S/ Peter Kemp, P.E.
DATE	PAVEMENT SUPERVISOR

D.D. 13 B 2-8b

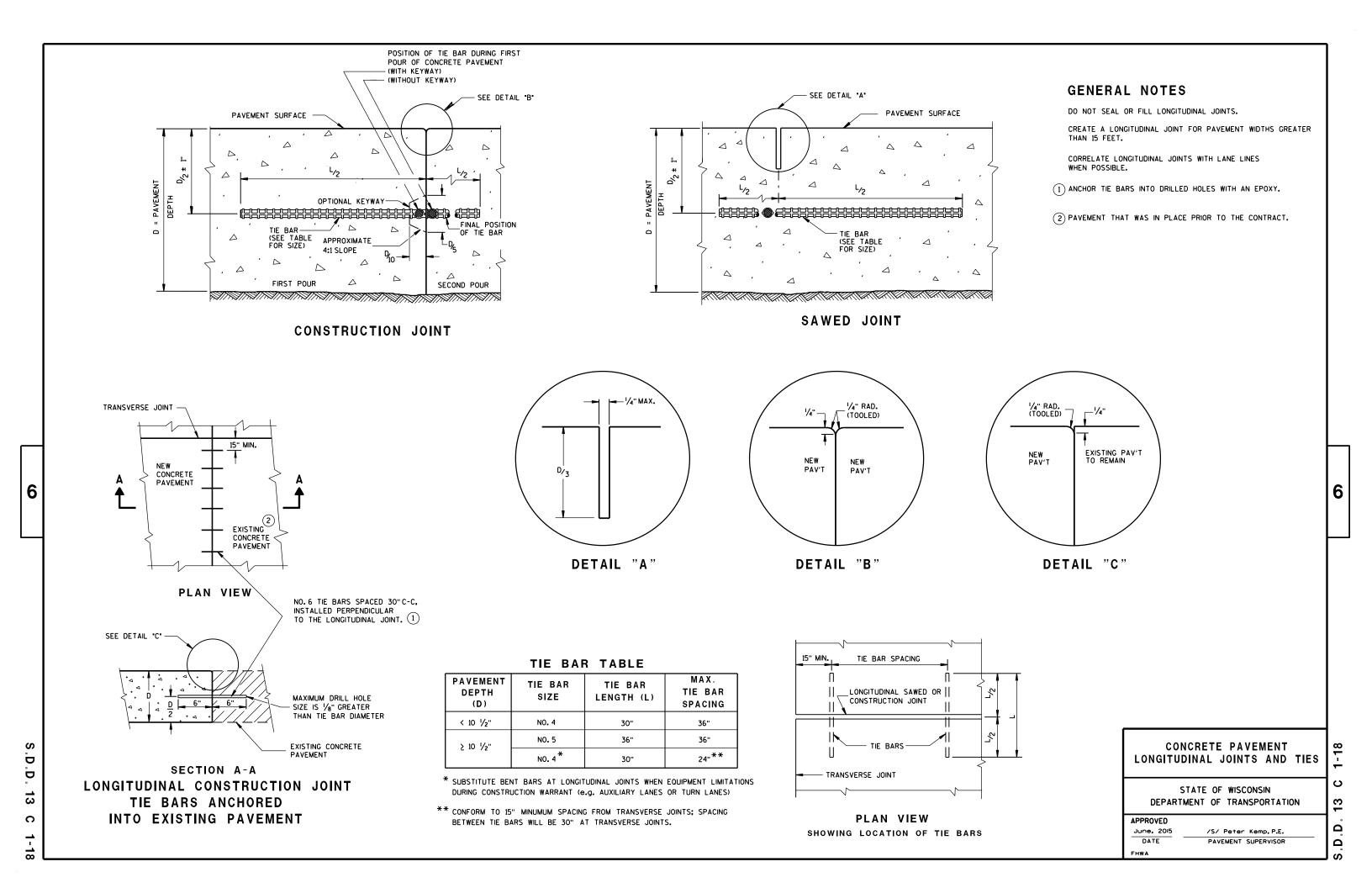
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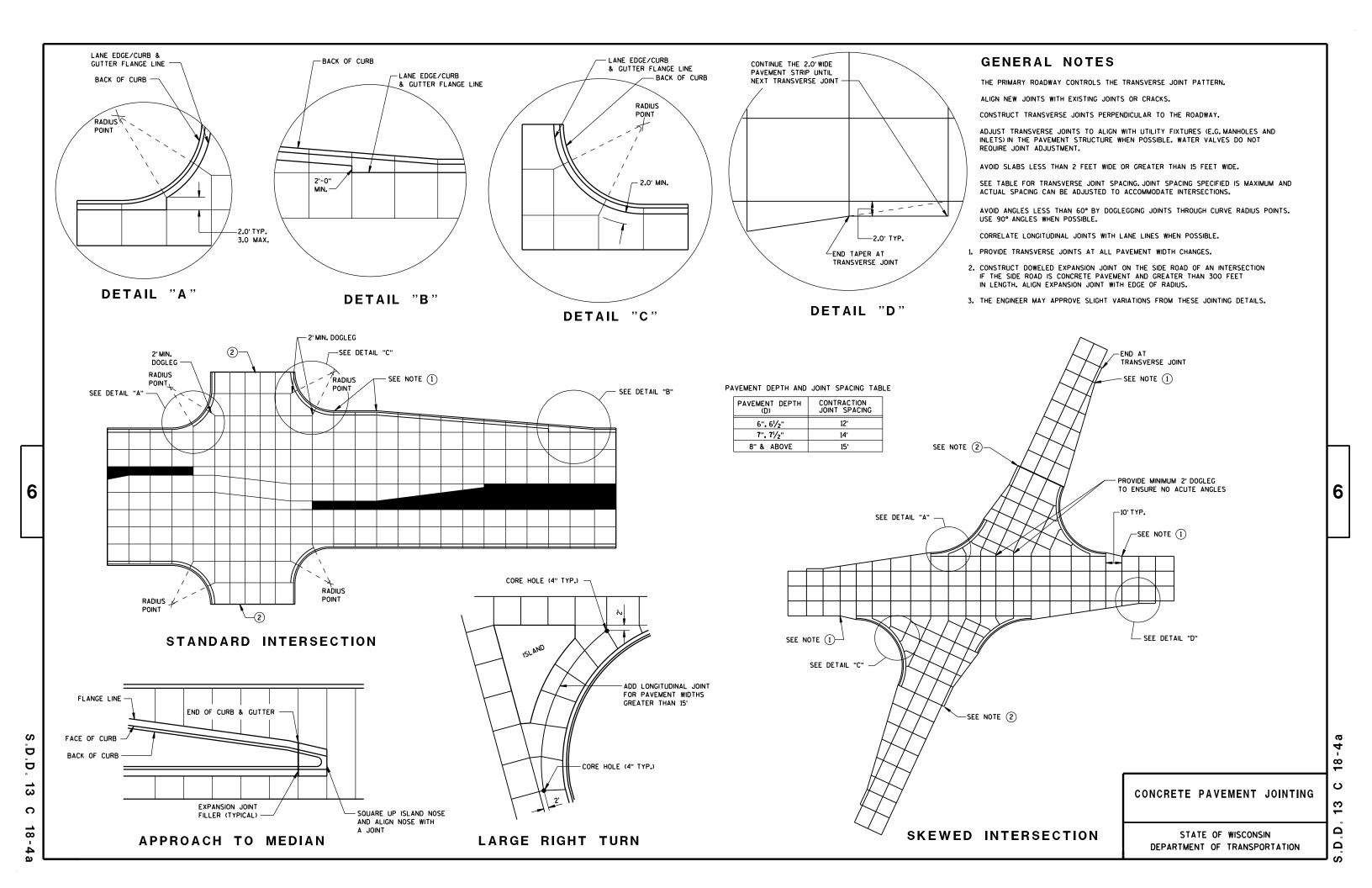
.D.D. 13

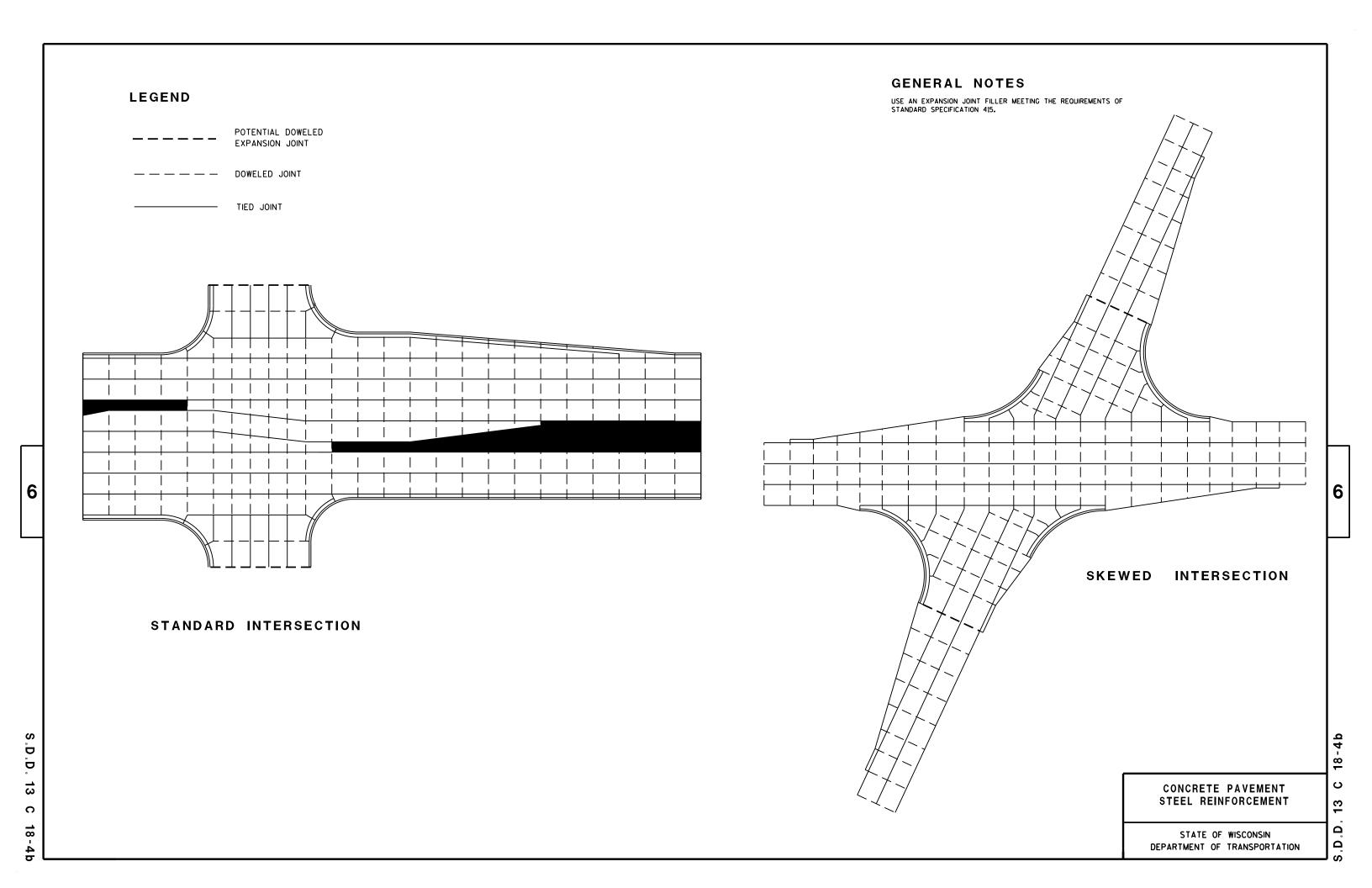
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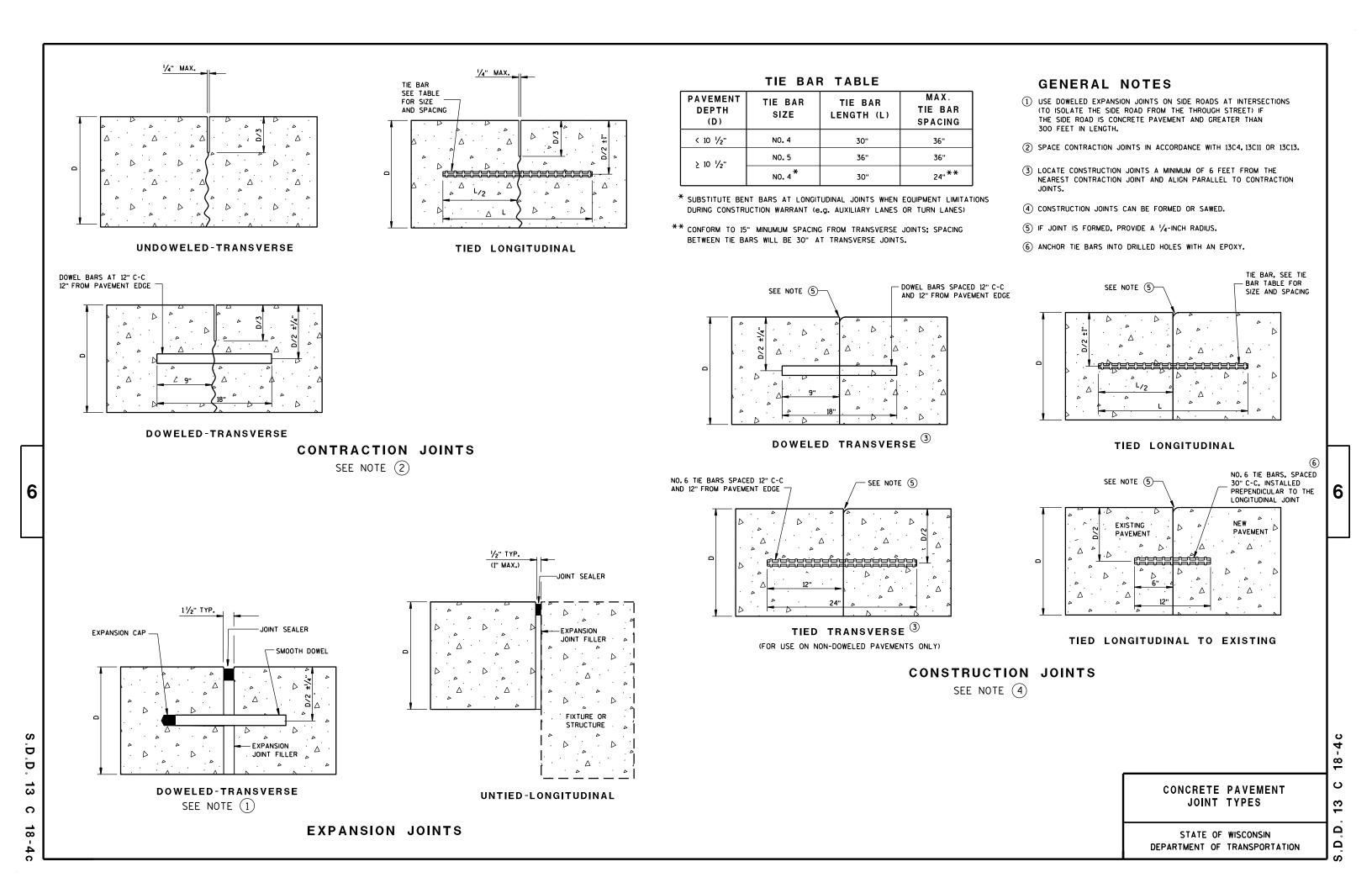
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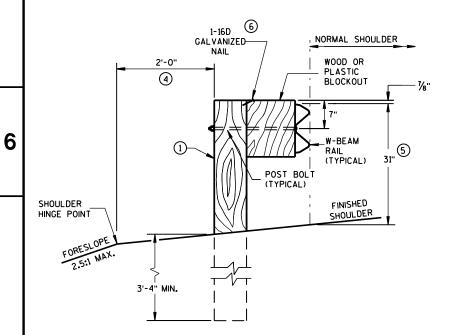






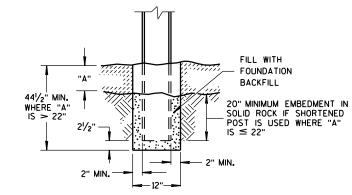


- 2 USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- (3) IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2½ INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- (4) WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- (5) FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS ± 1". FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 273/4" TO 32".
- (6) WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.



END VIEW

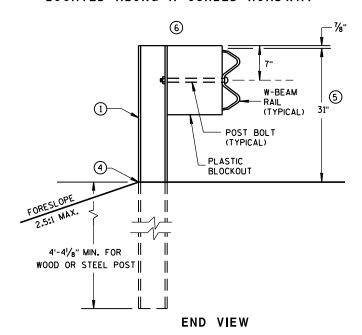
LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION



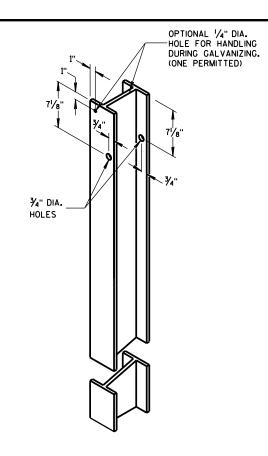
SETTING STEEL OR WOOD POST IN ROCK $^{\scriptsize{\textcircled{3}}}$



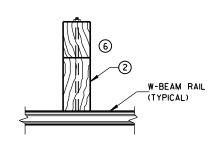
END VIEW
LOCATED ALONG A CURBED ROADWAY



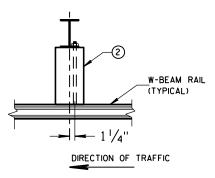
MGS LONGER POST AT HALFPOST SPACING W BEAM (K)



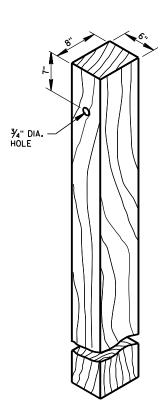
STEEL POST & HOLE PUNCHING DETAIL (w6X9)



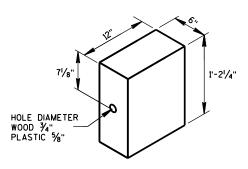
PLAN VIEW
WOOD POST,
BLOCKOUT & BEAM



PLAN VIEW
STEEL POST,
PLASTIC BLOCKOUT & BEAM



WOOD POST (6" X 8") NOMINAL $^{\scriptsize \textcircled{1}}$



WOOD OR PLASTIC BLOCKOUT

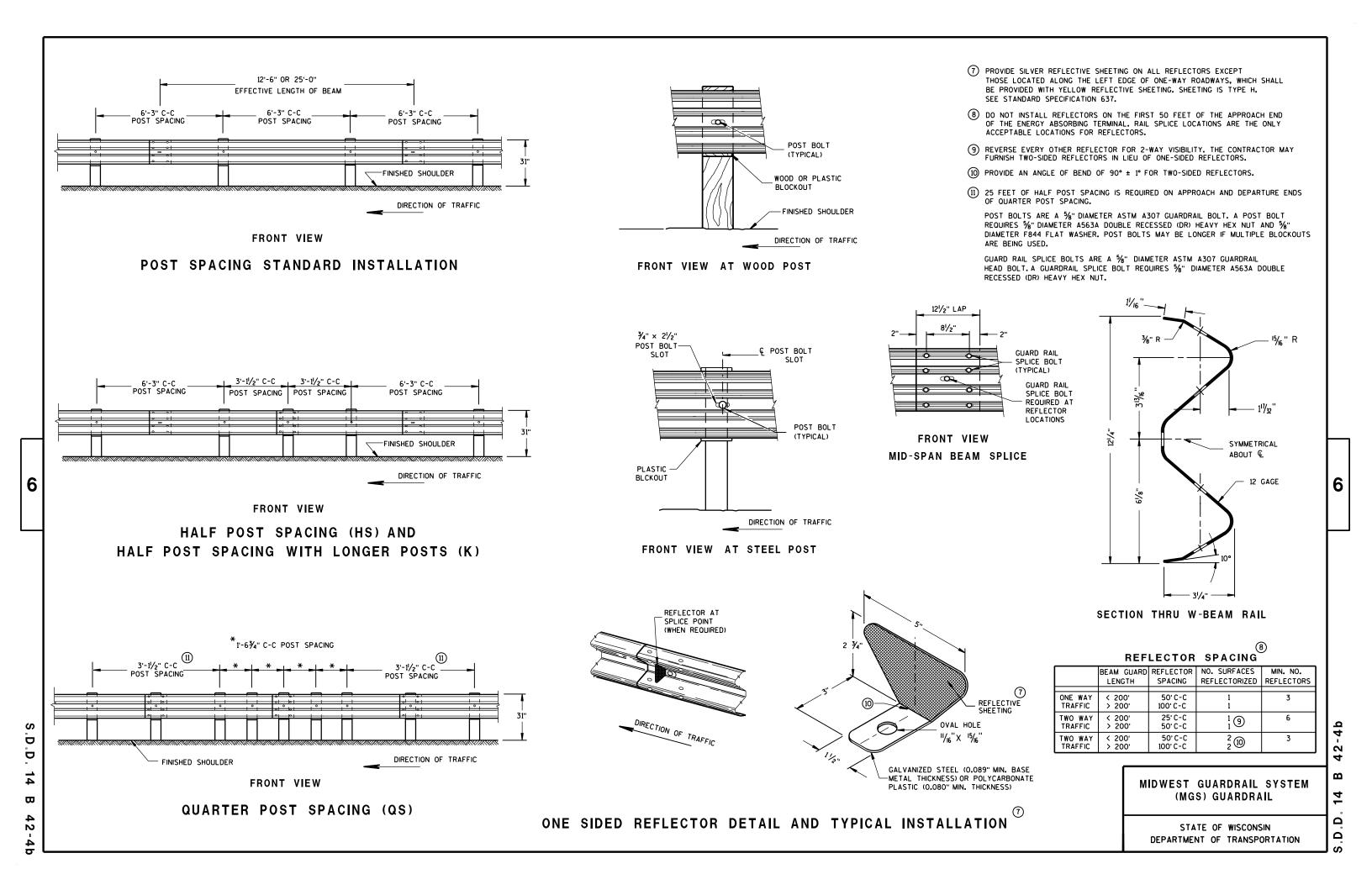
MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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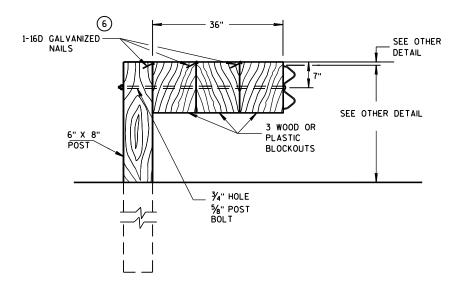
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DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

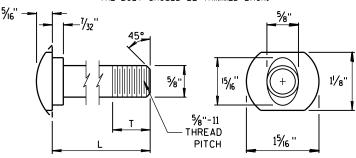


DETAIL FOR 36" BLOCKOUT DEPTH

NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.

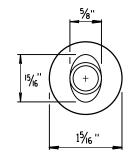
> DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

NOTE: 1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 1/16". 2. IF THE BOLT EXTENDS MORE THAN 1/4" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

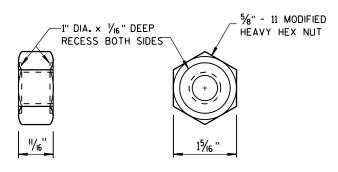


POST BOLT TABLE

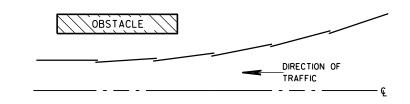
11/8"
-70
13/4"
4"
4½ ₆ "
4"
41/16"
4"



ALTERNATE BOLT HEAD

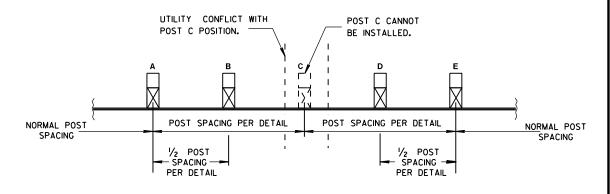


POST BOLT, SPLICE BOLT AND RECESS NUT



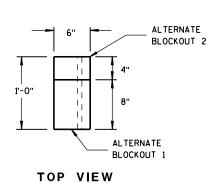
PLAN VIEW

BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION





SIDE VIEW

ALTERNATE WOOD **BLOCKOUT DETAIL**

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER

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SECTION A-A SECTION B-B

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PLAN VIEW

BILL OF MATERIALS

PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.		
1	WOOD BREAKAWAY POST		
2	6" X 8" X 0.188", 6'-0" LONG FOUNDATION TUBE AT POSTS 1AND 2		
3	WOOD CRT		
4	WOOD BLOCKOUT		
(5)	PIPE SLEEVE		
6	BEARING PLATE		
7	BCT CABLE ASSEMBLY		
8	ANCHOR CABLE BOX		
9	GROUND STRUT		
10	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.		
(11)	STANDARD W-BEAM RAIL.MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.		
12	END SECTION EAT		
(3)	0.040" ALUMINUM SHEET WITH REFLECTIVE SHEETING TYPE F PER SECTION 637 OF THE STANDARD SPECIFICATIONS		
14)	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)		



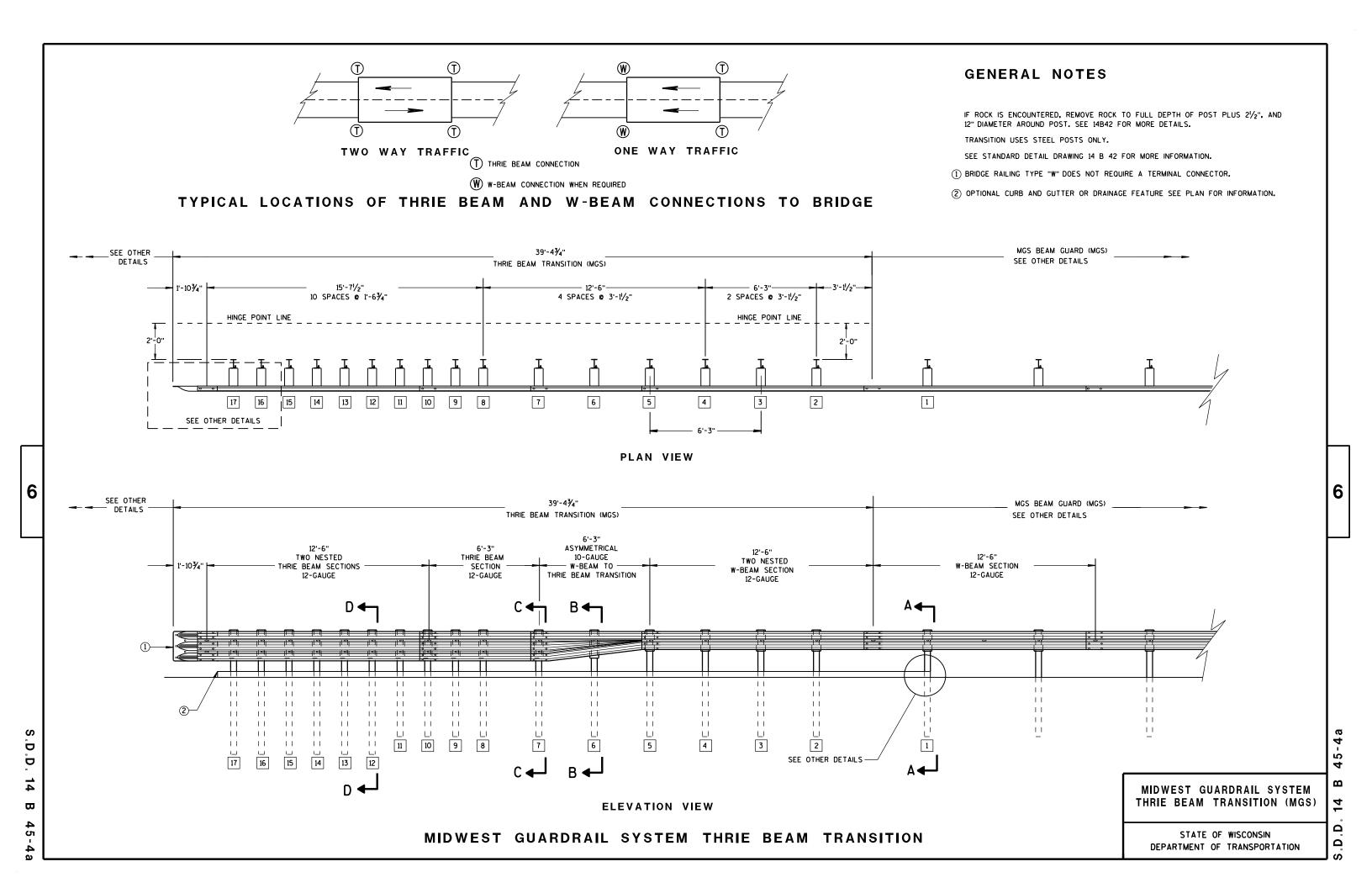
MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)

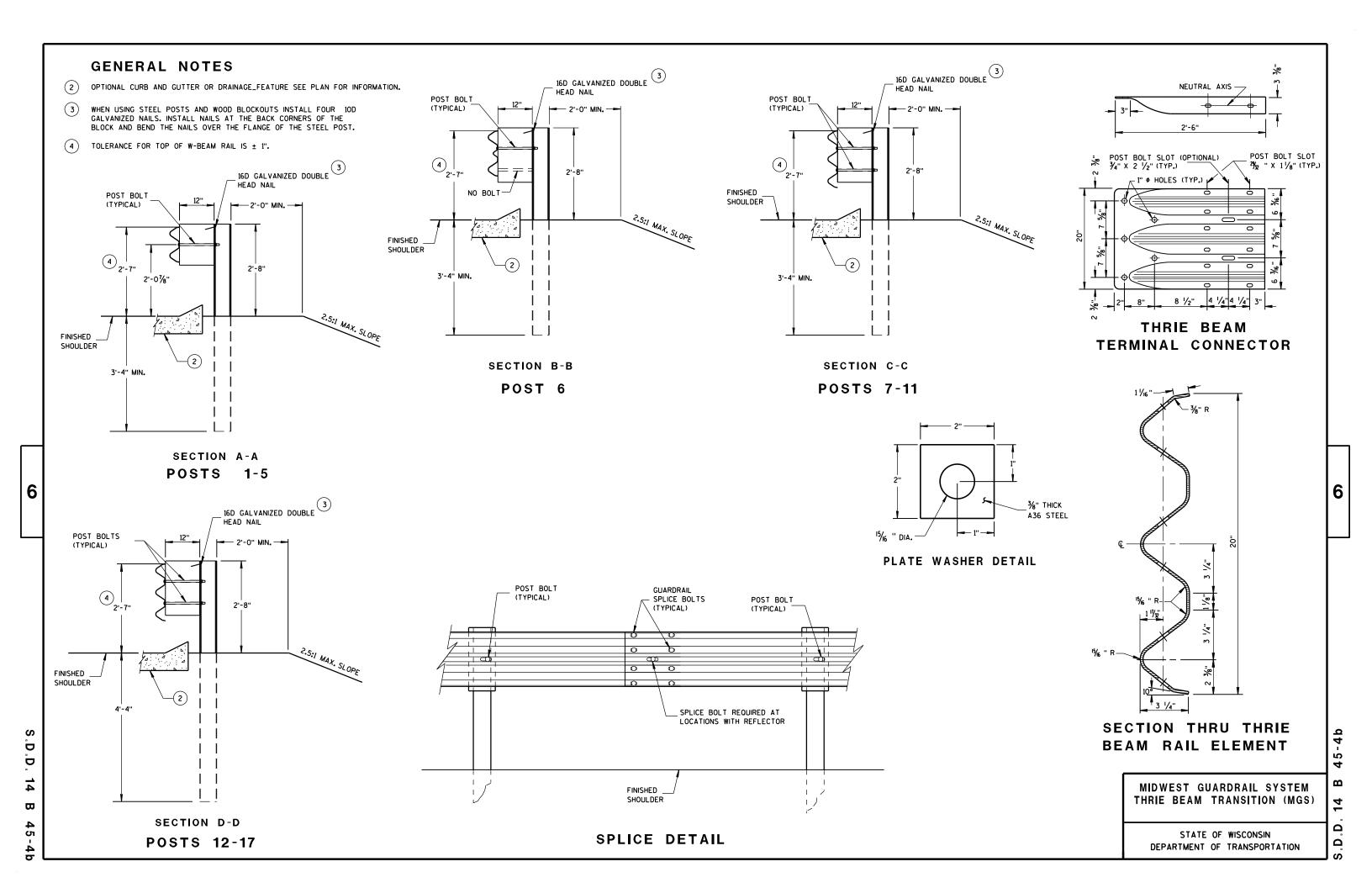
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

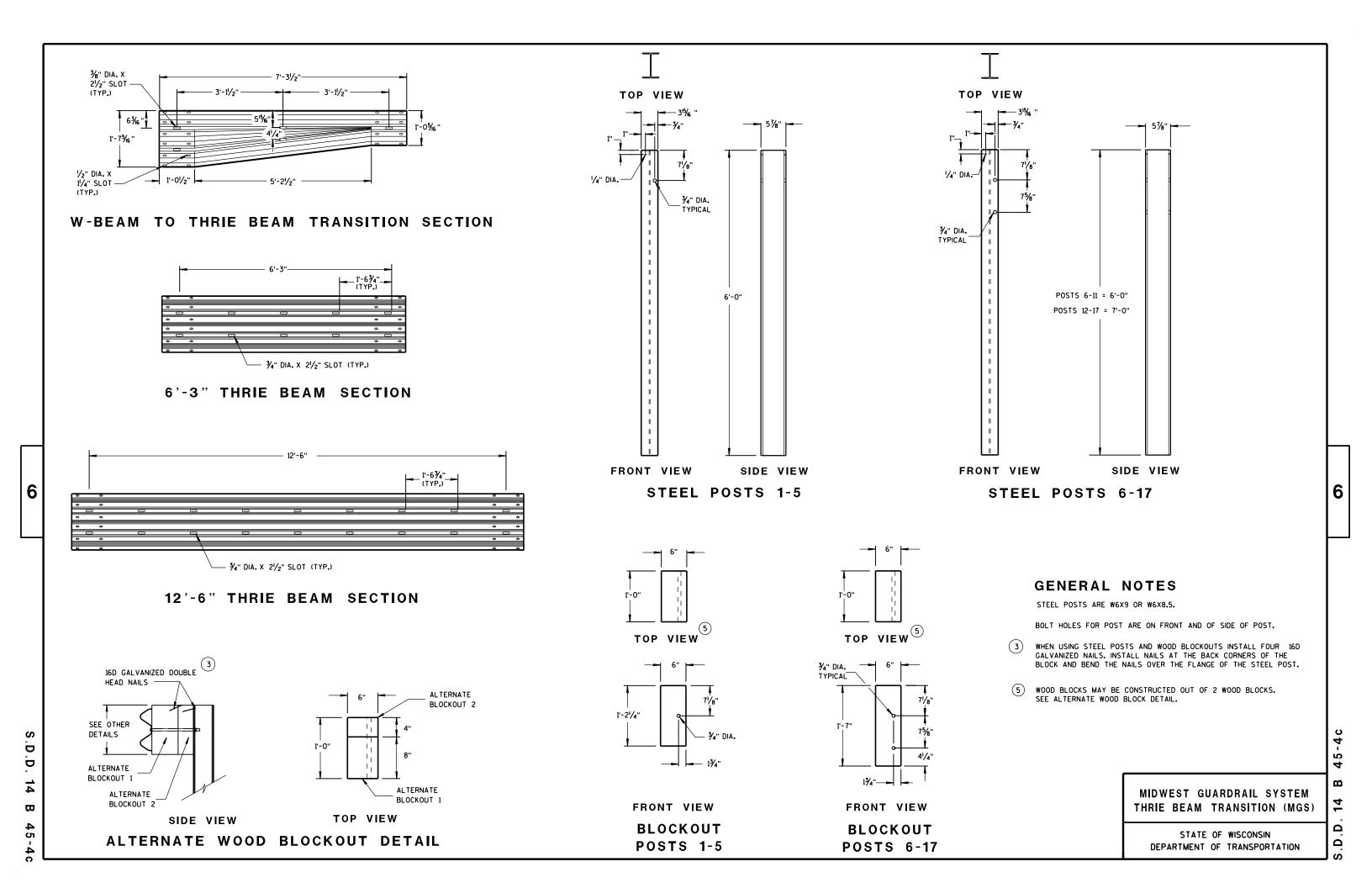
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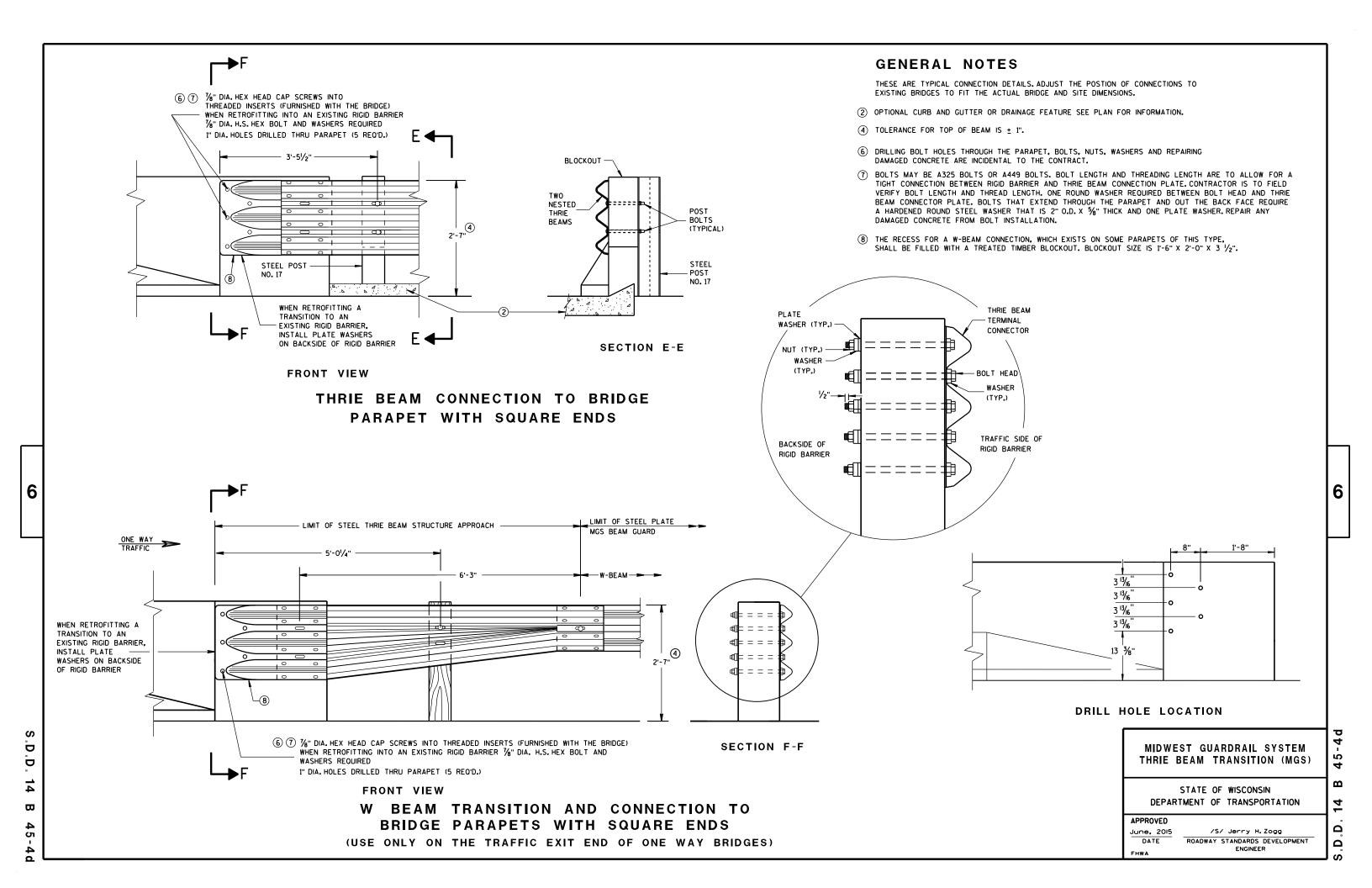
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THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSTION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

- (2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- (4) TOLERANCE FOR TOP OF BEAM IS ± 1".

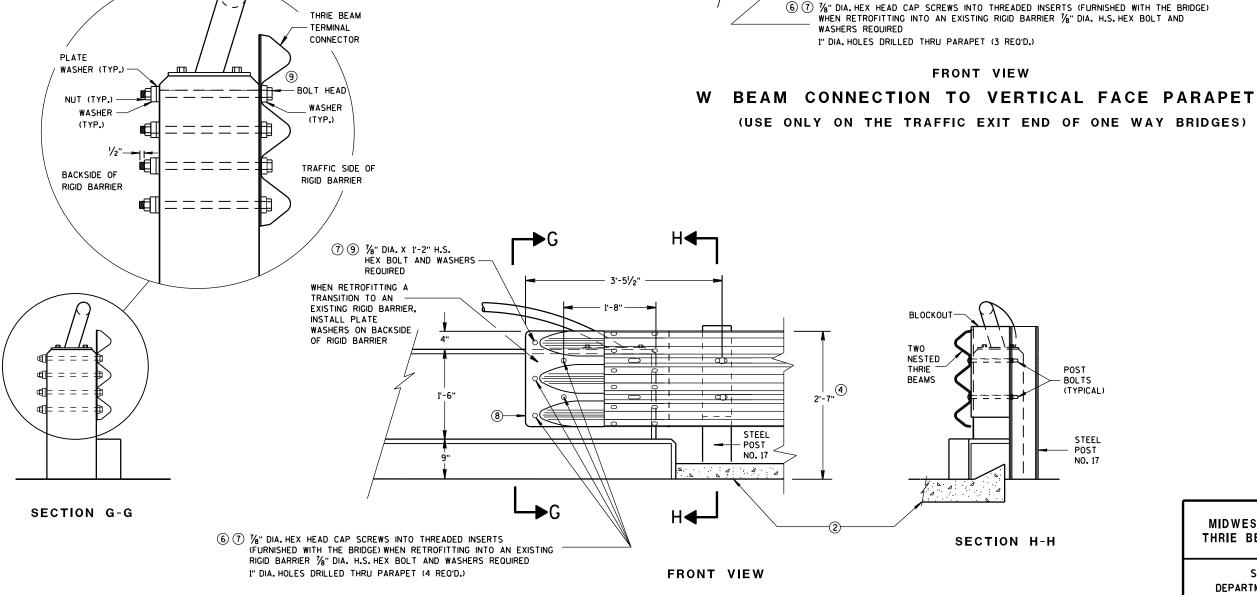
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- (6) DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.
- TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTION PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5%" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.
- (8) THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".
- (9) BOLT, NUT AND WASHERS NOT REQUIRED FOR THIS LOCATION WHEN RETROFITTING AN EXISTING PAPAPET AND THE HOLE IS EITHER ABOVE PARAPET OR WITHIN 4 INCHES OF THE EDGE OF PARAPET.



THRIE BEAM CONNECTION TO VERTICAL FACED PARAPETS

(7) 1/8" DIA. X 1'-2" H.S.

REQUIRED

WHEN RETROFITTING

A TRANSITION TO

AN EXISTING RIGID

BARRIFR, INSTALL

PLATE WASHERS

ON BACKSIDE OF

RIGID BARRIER

HEX BOLT AND WASHERS

W BEAM TERMINAL -

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MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2015
DATE
APPROVED
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVE

FHWA

LIMIT OF STEEL PLATE

MGS BEAM GUARD

ONE WAY

TRAFFIC

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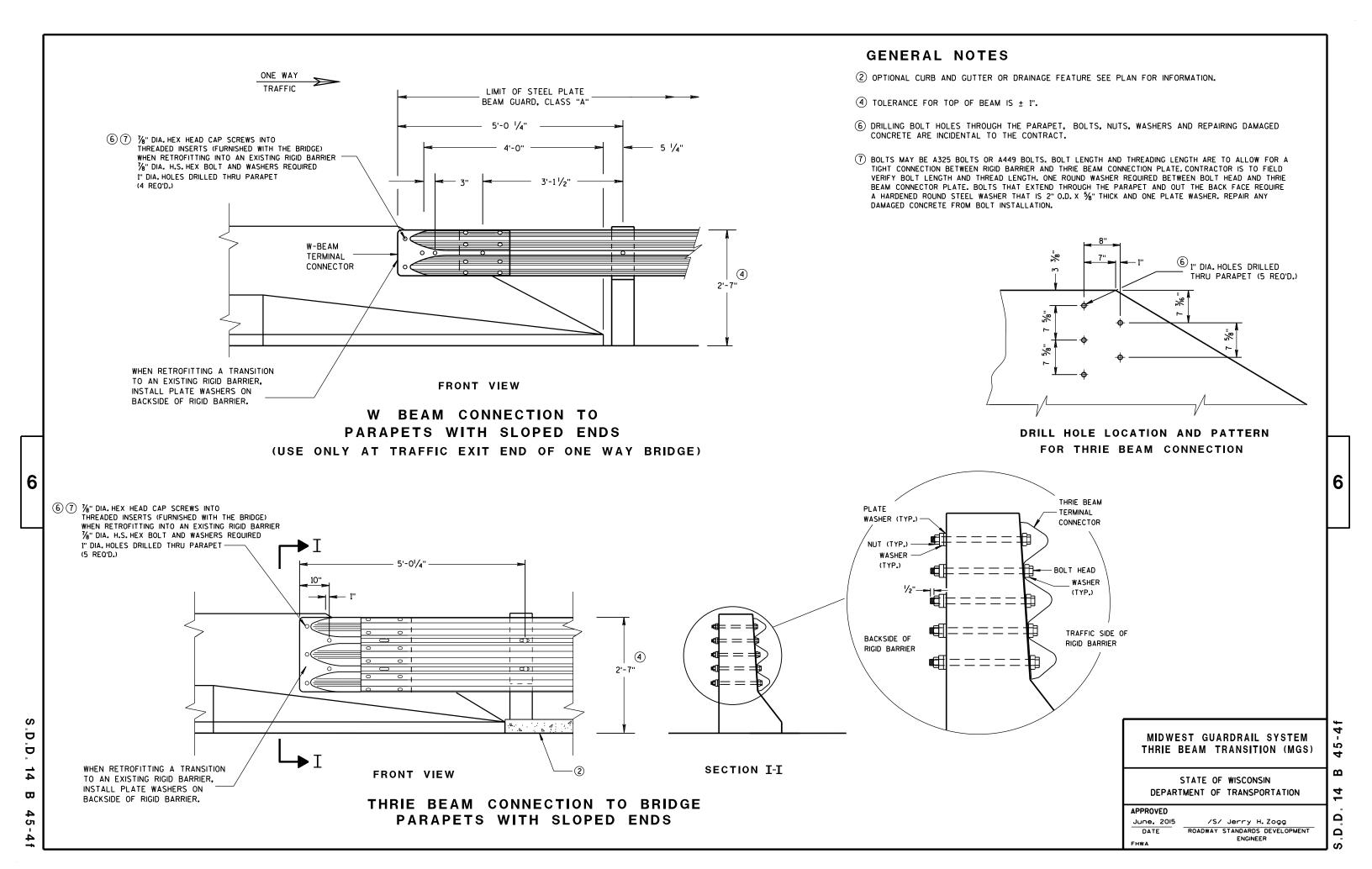
2'-7"

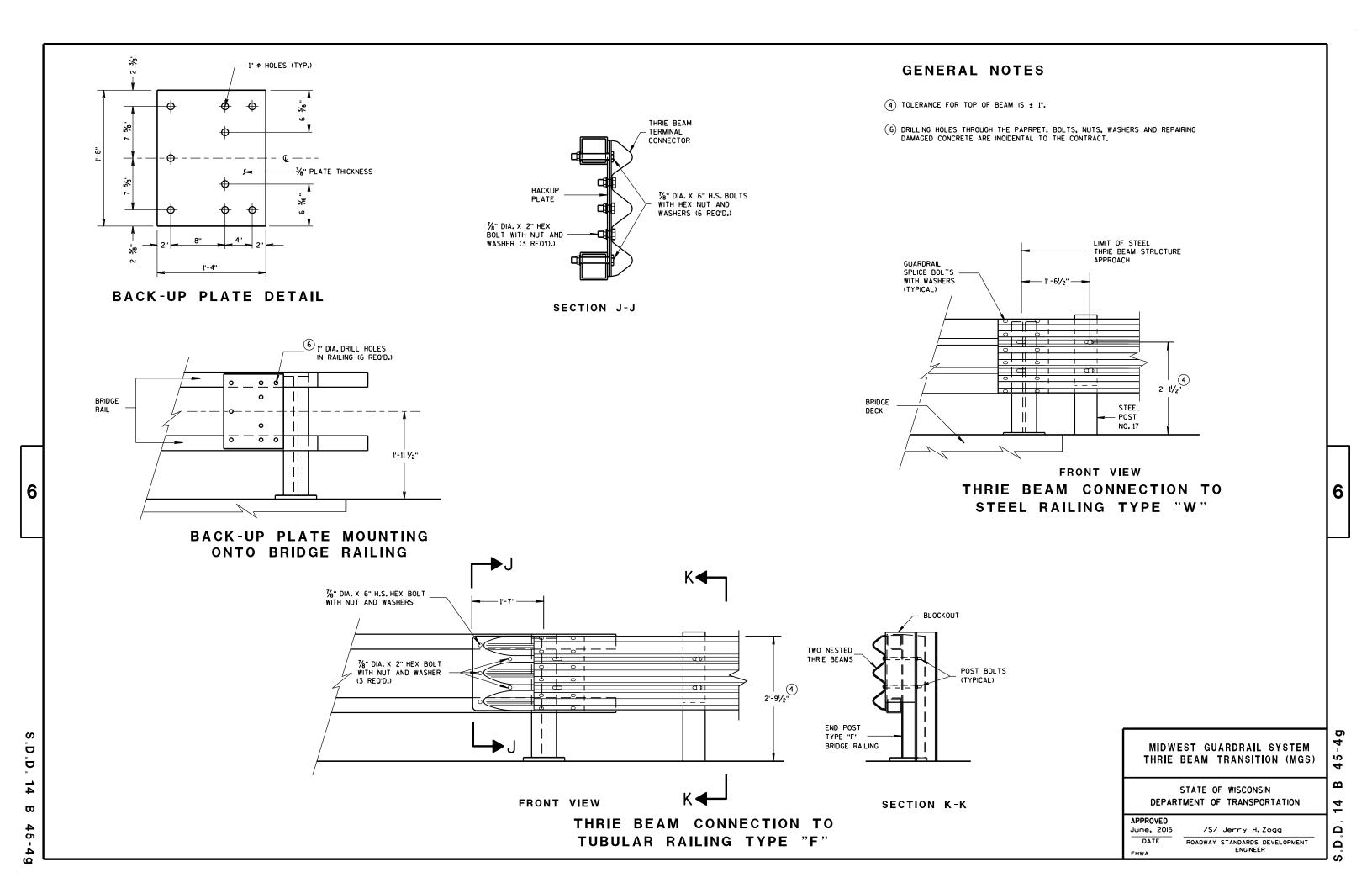
5'-0 1/4" —

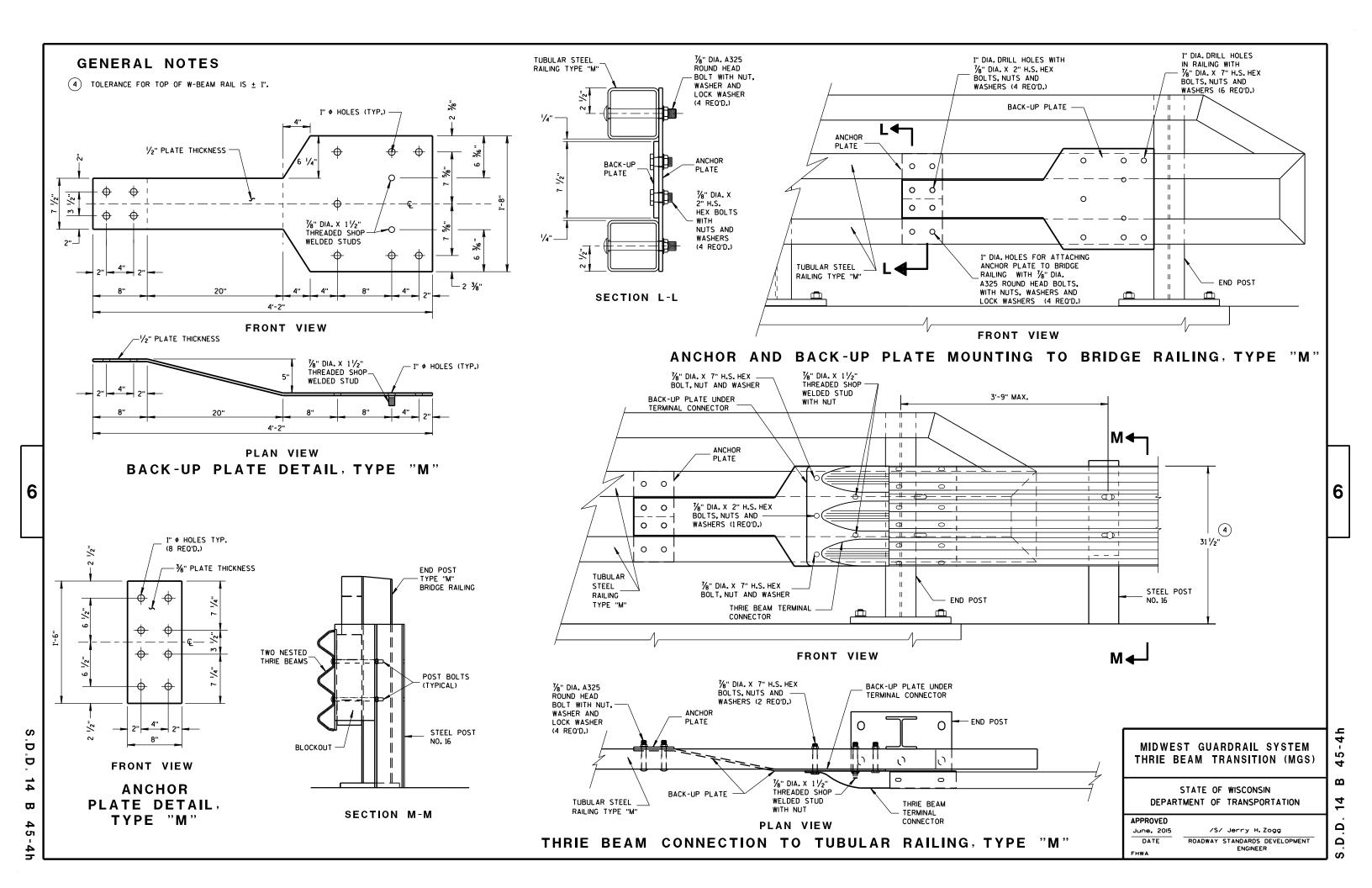
- 3'-1¹/₂"

ROADWAY STANDARDS DEVELOPMENT ENGINEER

S.D







	(PER ASSEMBLY)			
PLATE	QUANTITY	SHAPE	SIZE (A × B × C × D)	THICKNESS
P1	1	в₫	20" × 20"	3/6"
P2	1	B∱c	20" × 20" × 28%6"	¾6 "
Р3	1	B&D	39" × 35/8" × 20" × 195/6"	3/6 "
S1	4	B A	18 % 6" × 3 % " × 18 ¾ "	1/4"
S2	1	B D	10 ¹ / ₄ " × 2 ⁷ / ₁₆ " × 10 ³ / ₈ " × ¹ / ₂ "	1/4"
S3	1	B₽₽	3" × 1½6" × 3½" × ½"	1/4"
S4	1	в₫	61/8" × 21/16"	1/4"
S5	1	в₾	6½" × ½"	1/4"
S6	1	в₾	7¾" × 1¾"	1/4"
S7	1	A DC	2%6" × 6" × 35%" × 57%"	1/4"
S8	1	4 <u>0</u> 2	1 ⁵ / ₃₂ " × 7 ¹ / ₂ " × 2 ¹ / ₂ " × 7 ³ / ₈ "	1/4"
S9	1	C □ R	6½6" × 6¾6" × 1¾2"	1/4"
S10	1	A D C	11/8" × 91/8" × 35/8" × 911/16 "	1/4"
S11	1	c ≜	8½" × 8¾" × 1¼6 "	1/4"

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SINGLE SLOPE CONNECTION PLATE

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

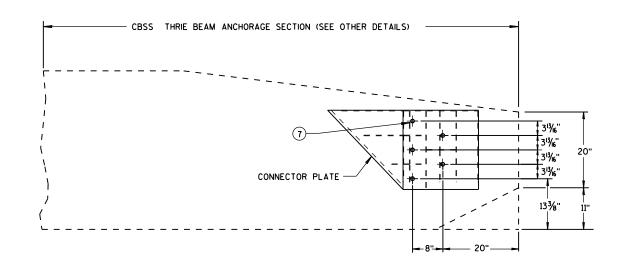
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2015	

/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER FHWA

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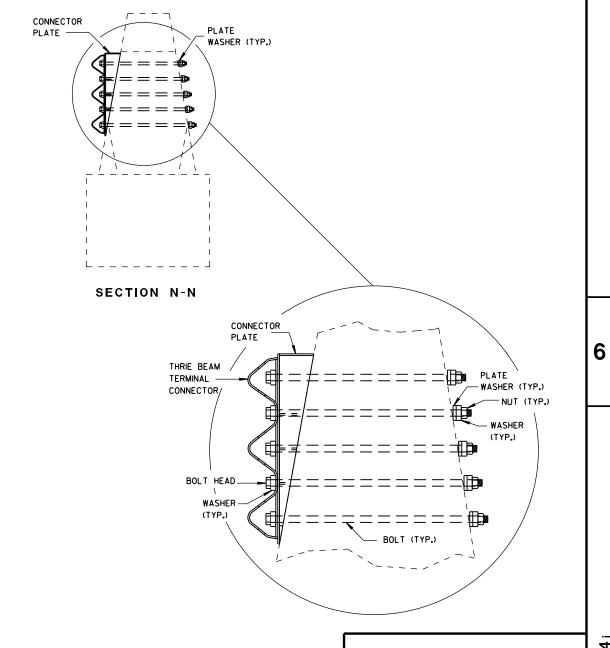


SINGLE SLOPE CONNECTION PLATE PLACEMENT

GENERAL NOTES

CONNECTOR PLATE, DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

- 2 OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X %" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.



MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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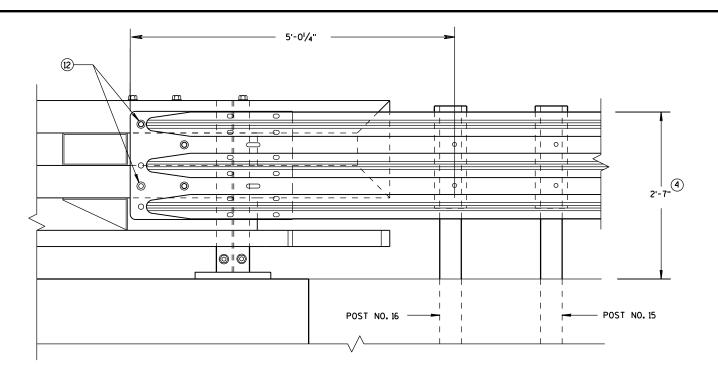
APPROVED
June, 2015 /S.

FHWA

OIS /S/ Jerry H. Zogg

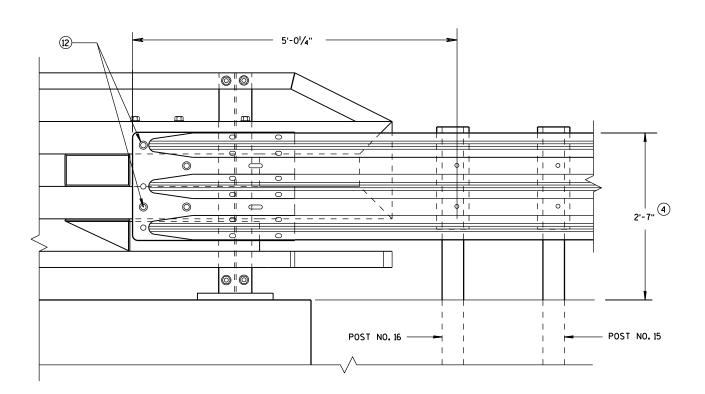
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

S.D.D. 14 B 4



ELEVATION OF DETAIL AT NY3 END POST

THRIE BEAM RAIL ATTACHMENT



ELEVATION OF DETAIL AT NY4 END POST

THRIE BEAM RAIL ATTACHMENT

GENERAL NOTES

- 4 TOLERANCE FOR TOP OF BEAM IS ± 1".
- (12) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND 1/2-INCH BEYOND NUT.

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS) 6

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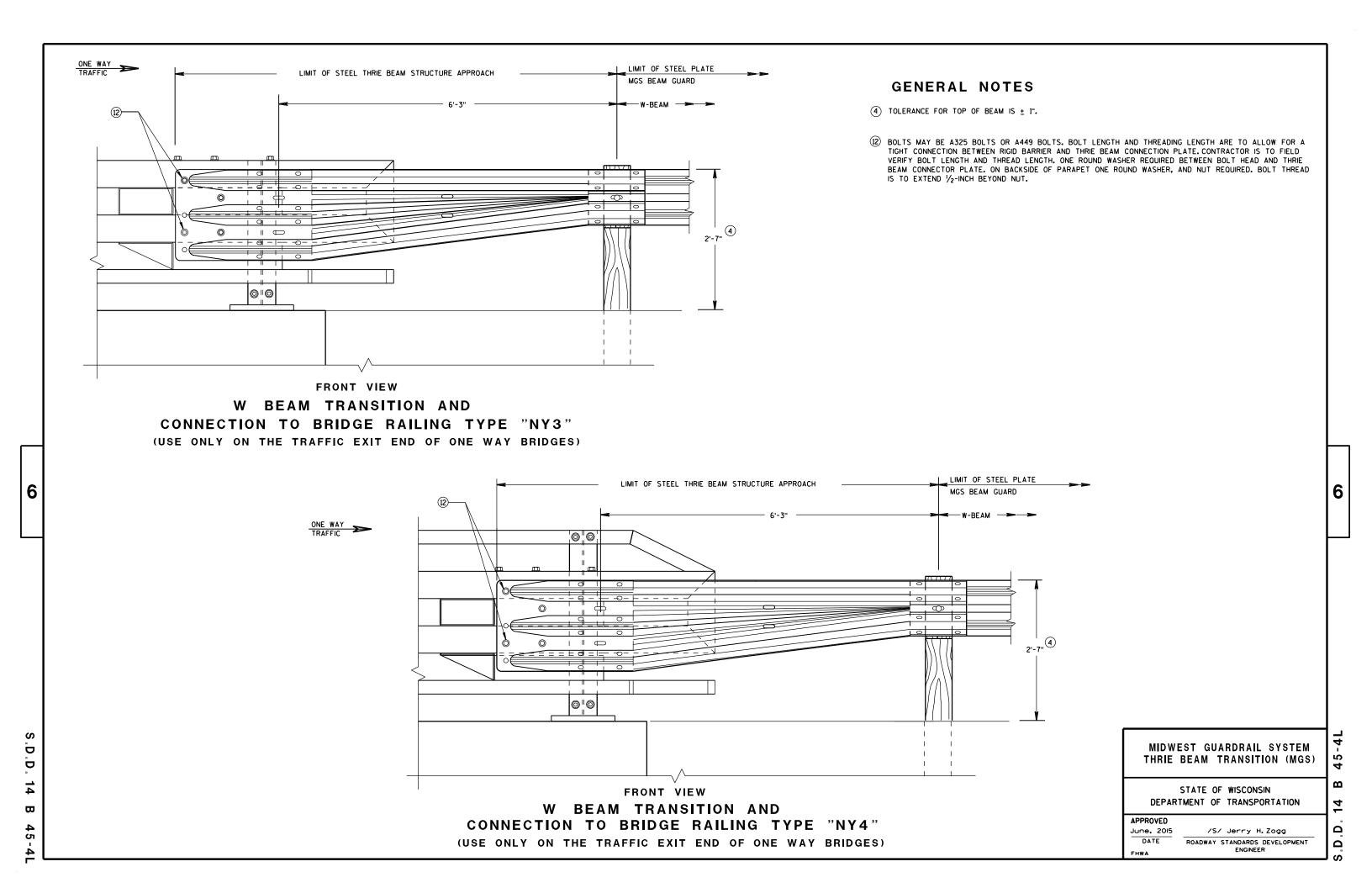
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

/S/ Jerry H. Zogg June, 2015 DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER FHWA

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ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E LANE CLOSURE BARRICADE DETAIL APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30". R11-3, R11-4 AND R10-61 SHALL BE 60" X 30". M4-9 SHALL BE 30" X 24". M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.) M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.) M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.) MO5-1 AND MO6-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.) D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS. R1-1 SHALL BE 36" X 36".

- (1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT
- THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS. PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

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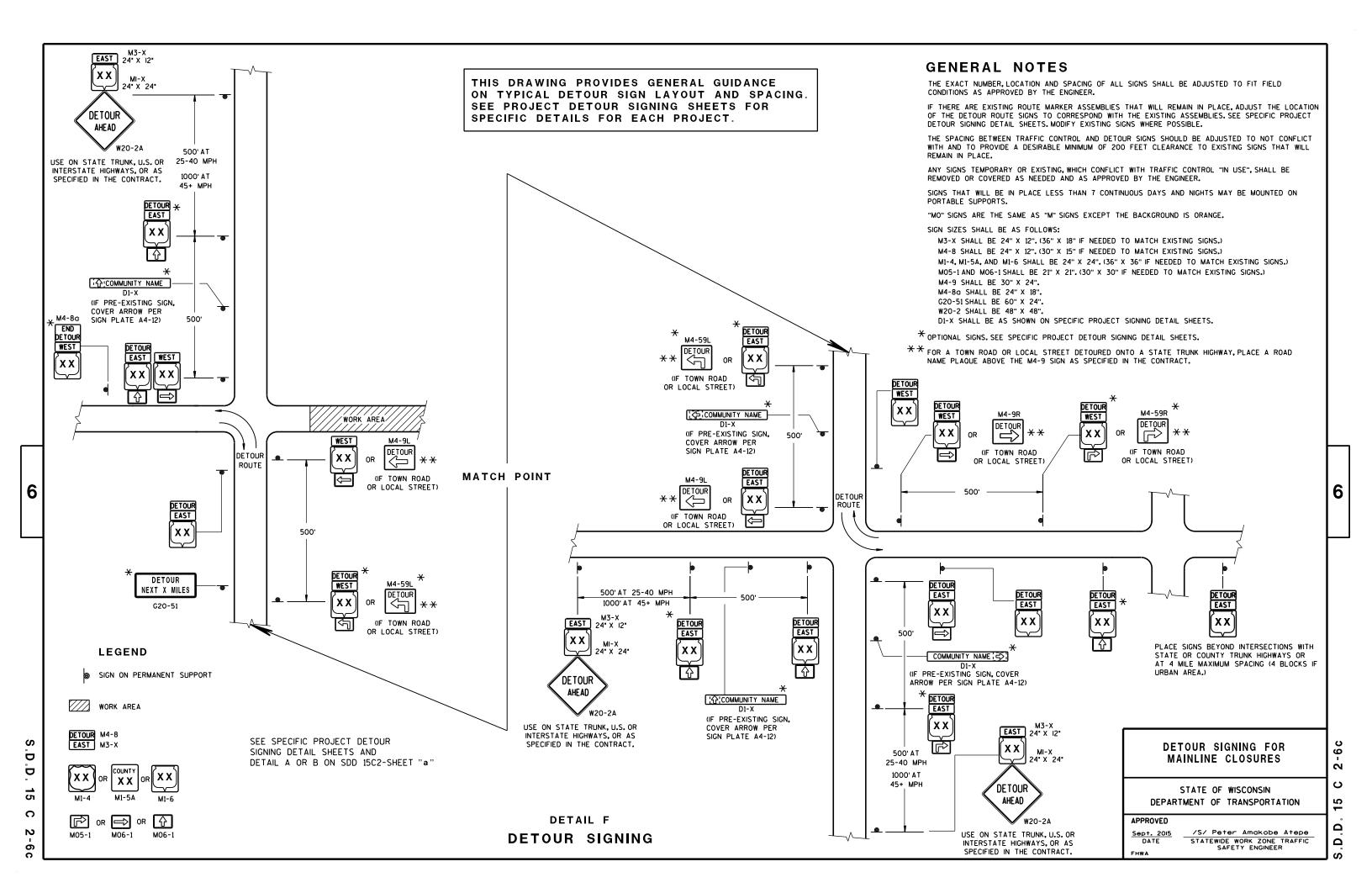
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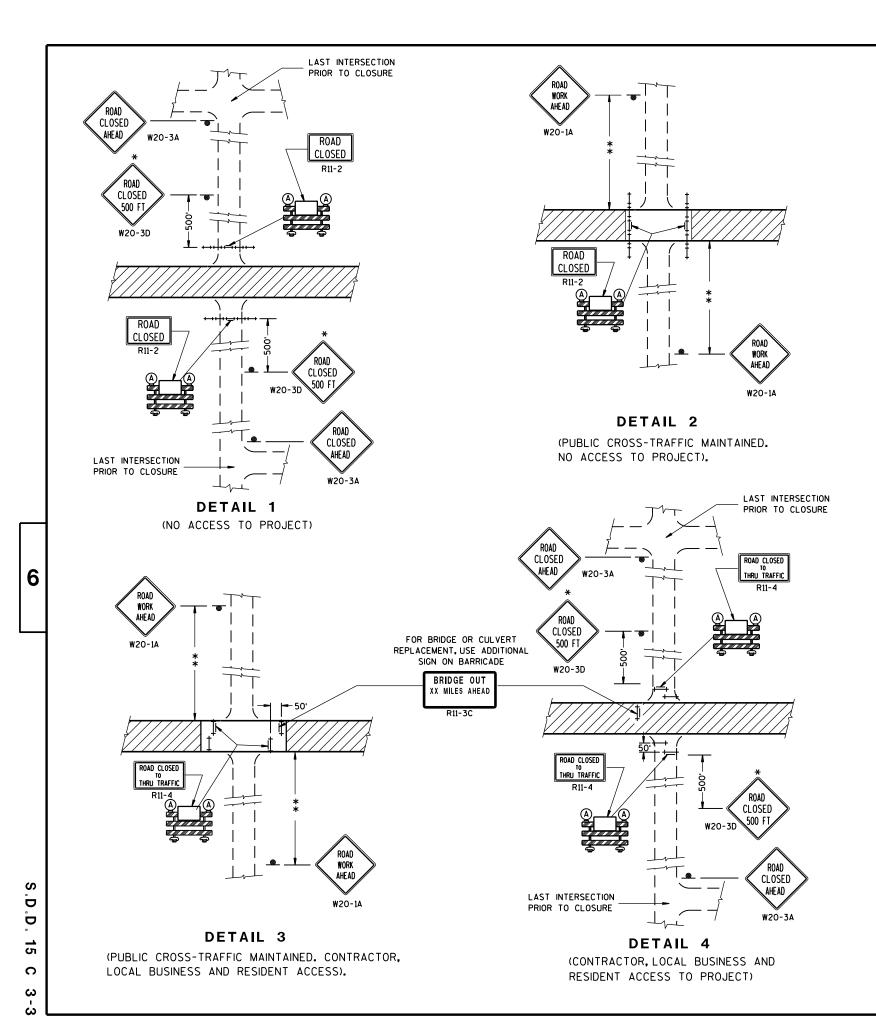
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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

/S/ Peter Amakobe Atepe

STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER





GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

SIGN ON PERMANENT SUPPORT

TYPE III BARRICADE

TYPE III BARRICADE WITH
ATTACHED SIGN

(A) TYPE "A" WARNING LIGHT (FLASHING)

WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

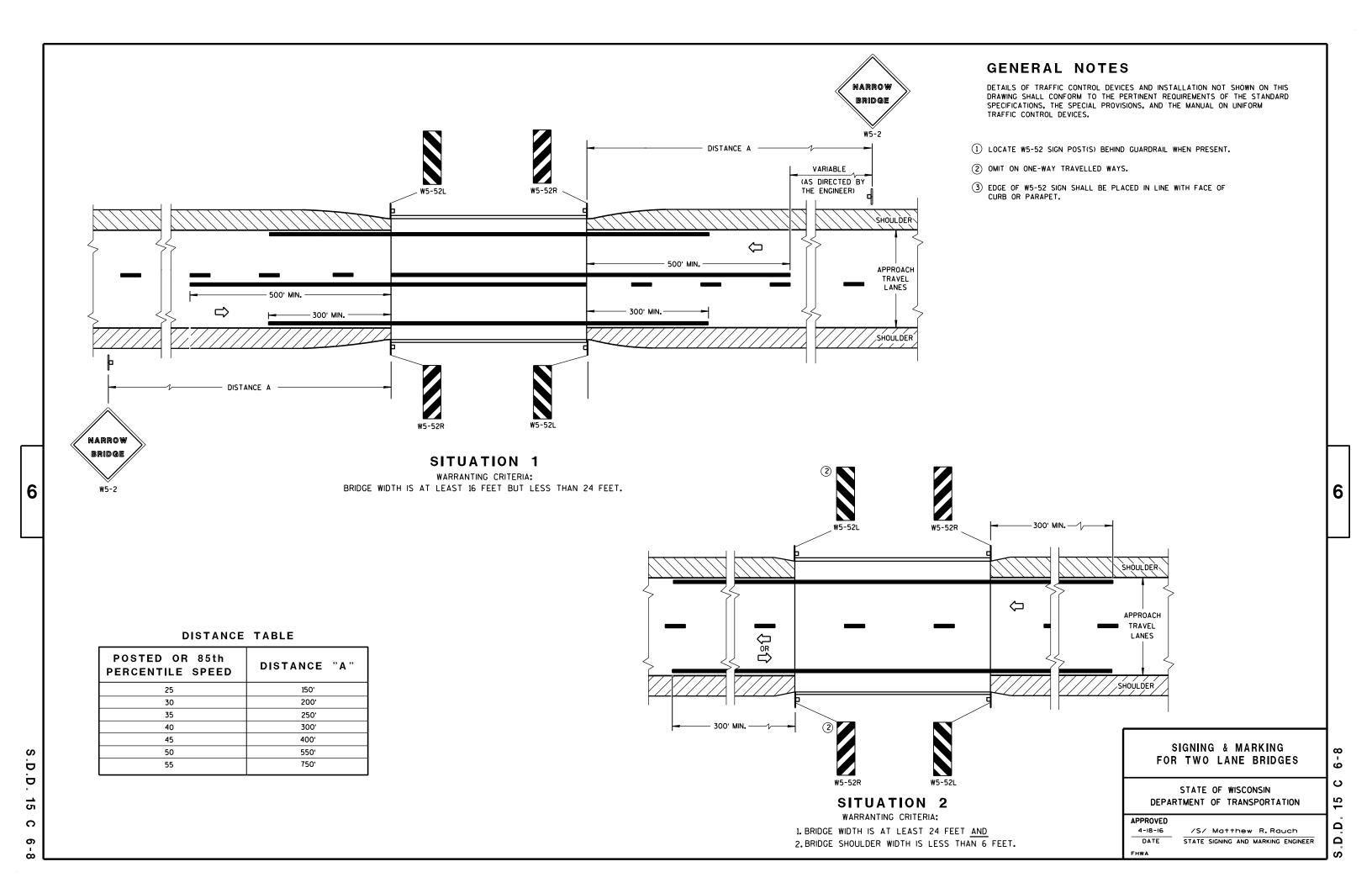
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

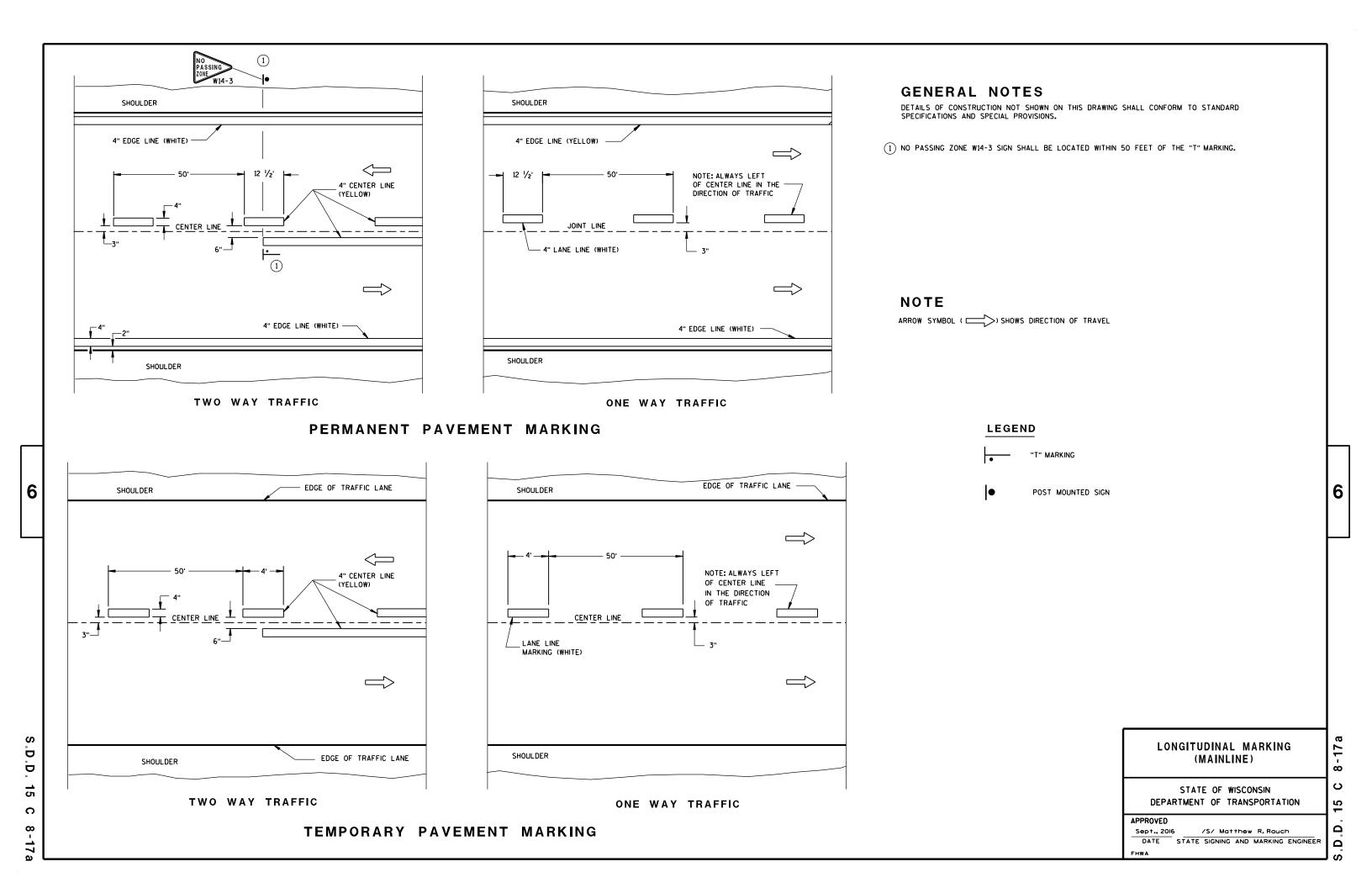
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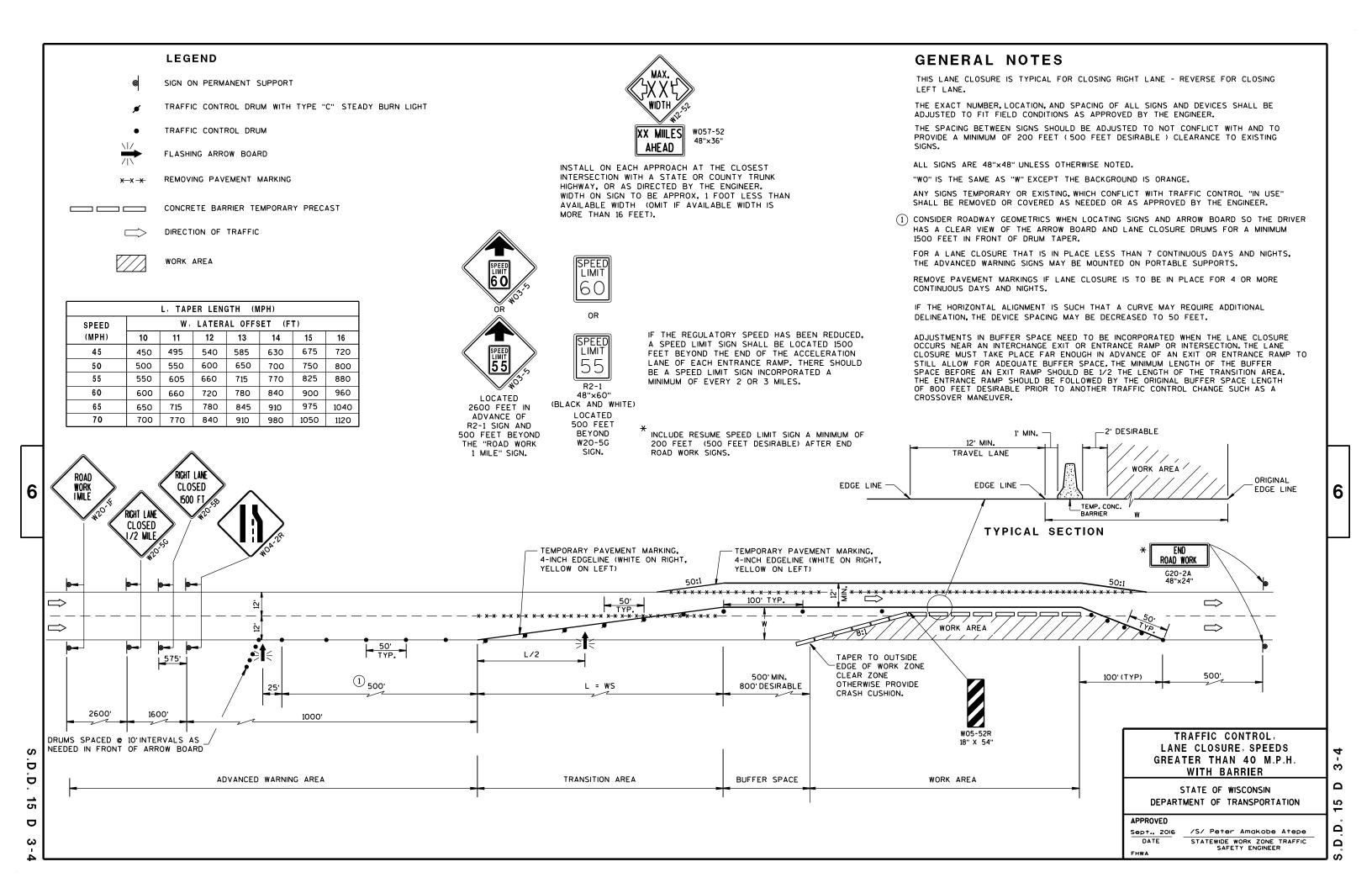
Sept. 2015

DATE
STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

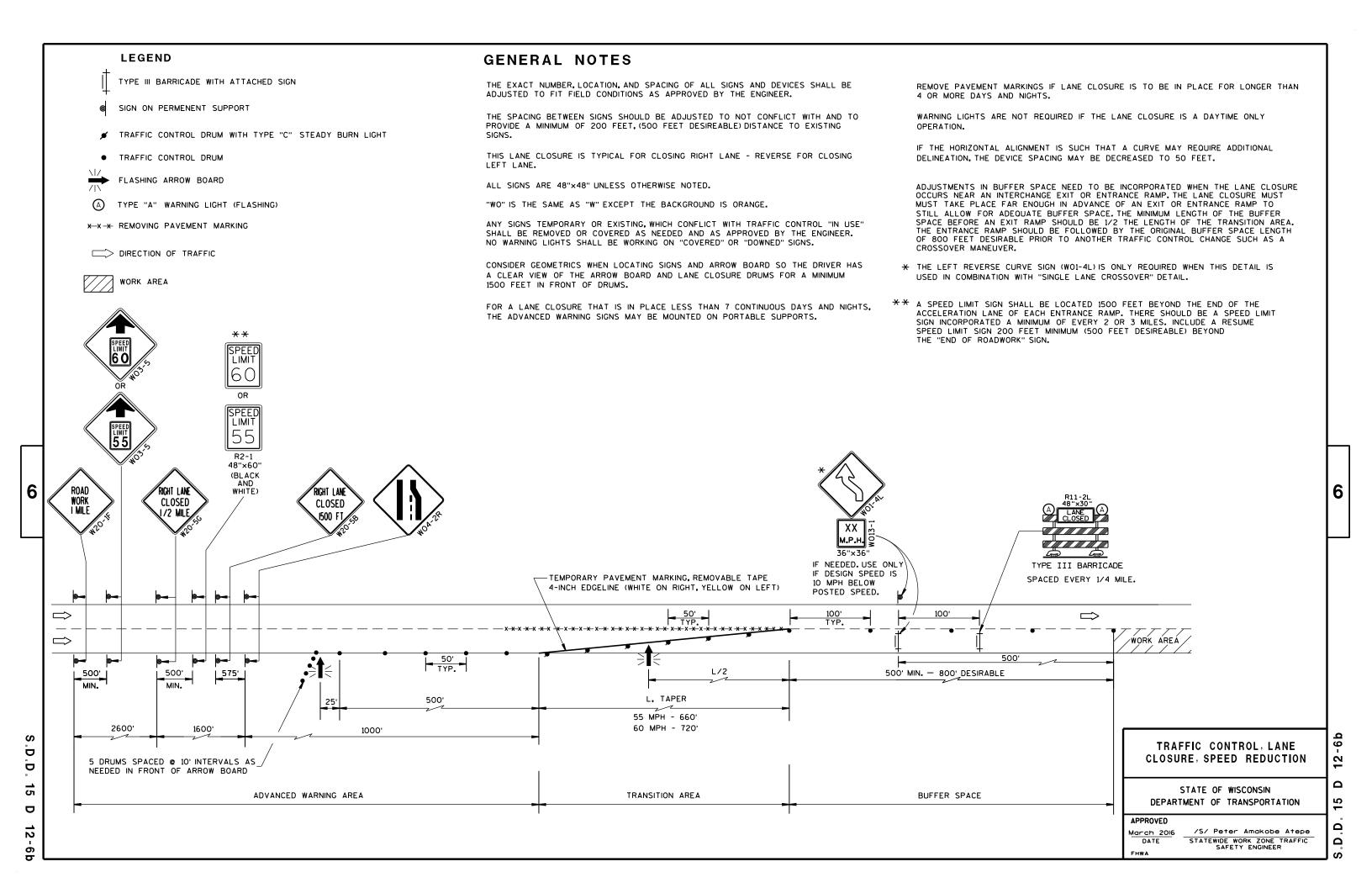
S.D.D. 15 C 3







GENERAL NOTES LEGEND THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR LONGER THAN ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER. 4 OR MORE DAYS AND NIGHTS. TYPE III BARRICADE WITH ATTACHED SIGN THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION. SIGN ON PERMENENT SUPPORT IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING DELINEATION. THE DEVICE SPACING MAY BE DECREASED TO 50 FEET. LEFT LANE. TRAFFIC CONTROL DRUM ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST FLASHING ARROW BOARD "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE. MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" TYPE "A" WARNING LIGHT (FLASHING) THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS. * X -X REMOVING PAVEMENT MARKING CROSSOVER MANEUVER. CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS * THE LEFT REVERSE CURVE SIGN (WO1-4L) IS ONLY REQUIRED WHEN THIS DETAIL IS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM USED IN COMBINATION WITH "SINGLE LANE CROSSOVER" DETAIL. DIRECTION OF TRAFFIC 1500 FEET IN FRONT OF DRUMS. FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS. THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS. 6 6 WORK CLOSED CLOSED I MILE 1500 F XX м.Р.н 36"×36" IF NEEDED. USE ONLY TYPE III BARRICADE IF DESIGN SPEED IS TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE SPACED EVERY 1/4 MILE. 10 MPH BELOW 4-INCH EDGELINE (WHITE ON RIGHT, YELLOW ON LEFT) POSTED SPEED. 100' \Rightarrow \Rightarrow \Longrightarrow WORK AREA 50' L/2 500' MIN. - 800' DESIRABLE 575 L. TAPER 500 50 MPH - 600' 55 MPH - 660' 2600' 1600' 1000' 60 MPH - 720' TRAFFIC CONTROL, 9 65 MPH - 780' D 70 MPH - 840' LANE CLOSURE 5 DRUMS SPACED @ 10' INTERVALS AS 2 Ö NEEDED IN FRONT OF ARROW BOARD 15 Δ STATE OF WISCONSIN ADVANCED WARNING AREA TRANSITION AREA BUFFER SPACE DEPARTMENT OF TRANSPORTATION D **APPROVED** /S/ Peter Amakobe Atepe 2 March 2016 STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER Ω 6 FHWA

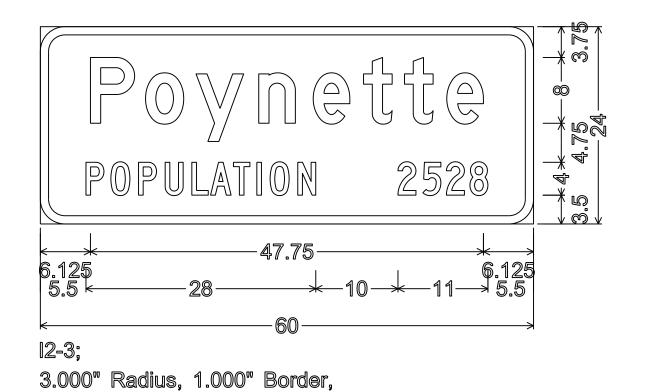


NOTES

- 1. All Signs Type II Type H Reflective
- 2. Color:

Background - GREEN Message - WHITE

3. Message Series - D except as Shown



PROJECT NO: 6020-04-81

HWY: USH 51

COUNTY: COLUMBIA

PERMENANT SIGNING

SHEET NO:

Ε

FILE NAME · C·\CAFfiles\Projects\tr d1\1112an16 DGN

PLOT DATE . 10-NOV-2016 16:59

"Poynette" D; "POPULATION" C; "2528" C

PLOT RY . \$\$ plotuser \$\$ PLOT NAME :

URBAN ARFA



RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



5'-3"(生) D^{-1} Outside Edae of Gravel

White Edgeline Location

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where

there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

PLOT BY : mscj9h

GENERAL NOTES

- 1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is $7'-3''(\pm)$ or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A2-1S) is $7'-3''(\pm)$ or $6'-3''(\pm)$ per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is $5' - 3'' (\pm)$.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (+) tolerance for mounting height is 3 inches.
- 8. Folding signs shall be mounted at a height of 5'-3'' (\pm) or as directd by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm) . The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3'' (\pm).

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

APPROVED

for State Traffic Engineer

DATE 7/23/15

PLATE NO. <u>A4-3.20</u>

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A43.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:21

COUNTY:

PLOT NAME :

PLOT SCALE: 99.237937:1.000000

WISDOT/CADDS SHEET 42



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

WISDOT/CADDS SHEET 42

GENERAL NOTES

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3'' (±) or 6'-3'' (±) per urban or rural detail respectively.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).
- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- *** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

URBAN AREA RURAL AREA (See Note 3) 2'Min - 4'Max (See Note 6) ₩E# FF# 6'-3"(±) 6'-3"(±) 7'-3"(±) ** Curb ********\ Flowline D **7000** White Edgeline D 11 White Edgeline, Location Outside Edae Location

2' Min - 4' Max (See Note 6) 6'-3"(±) Curb Flowline. - 11

48" DIAMOND WARNING SIGN

HWY:

_ 26" 5 ' - 3 "(±) White Edgeline Location Outside Edge of Gravel 48" DIAMOND WARNING SIGN

COUNTY:

Outside Edge

of Gravel

	SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E	
* * *	Greater than 48" Less than 60"	12"	
	60" to 120"	L/5	l

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)		
L	E	
Greater than 120" less than 168"	12"	

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)		
L	E	
168" and greater	12"	

POST EMBEDMENT DEPTH

of Gravel

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

Matther

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:23

PLOT SCALE : 107.021305:1.000000

WISDOT/CADDS SHEET 42

PLOT NAME :

PLOT BY: mscj9h

WISCONSIN DEPT OF TRANSPORTATION APPROVED

For State Traffic Engineer

PLATE NO. 44-4.14 DATE 7/23/15



Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Nather R Raw
For State Traffic Engineer

DATE <u>8/11/16</u>

PLATE NO. <u>44-8.8</u>

PROJECT NO:

FILE NAME : C:\CAFfiles\Projects\tr stdplote\A48 DCN

PLOT DATE . 11-416-2016 11:35

PINT RY * \$\$ nintuser \$\$

SHEET NO:

| | |



PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

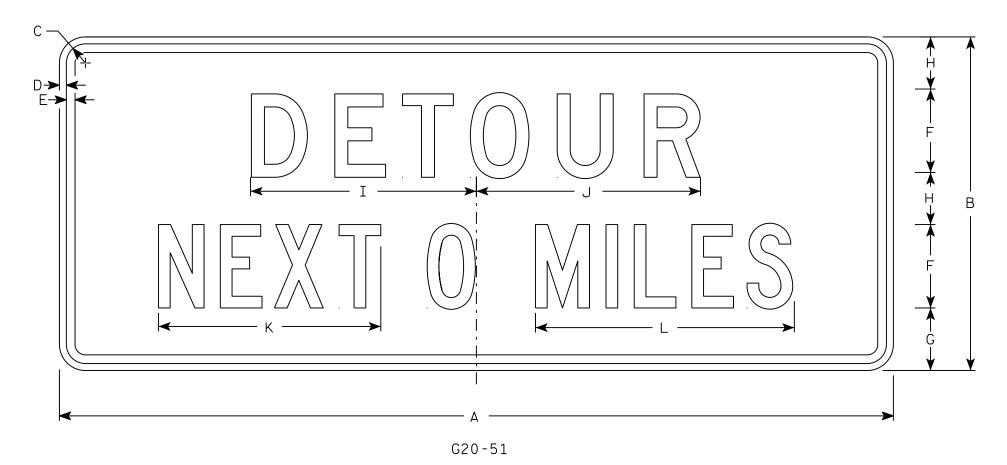
For State Traffic Engineer



- 1. Sign is Type II Type F Reflective
- 2. Color:

Background - Orange Message - Black

- 3. Message Series Line 1 is D and Line 2 is C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance



SIZE Z D Ε 4 1/2 3 3/4 16 1/4 16 1/8 24 | 1 3/8 1/2 5/8 16 18 5/8 6 10 3 24 1 3/8 5/8 4 1/2 3 3/4 16 1/4 16 1/8 1/2 60 6 16 18 5/8 10

COUNTY:

STANDARD SIGN G20-51

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

ROVED Matthew & Rauch
For State Traffic Engineer

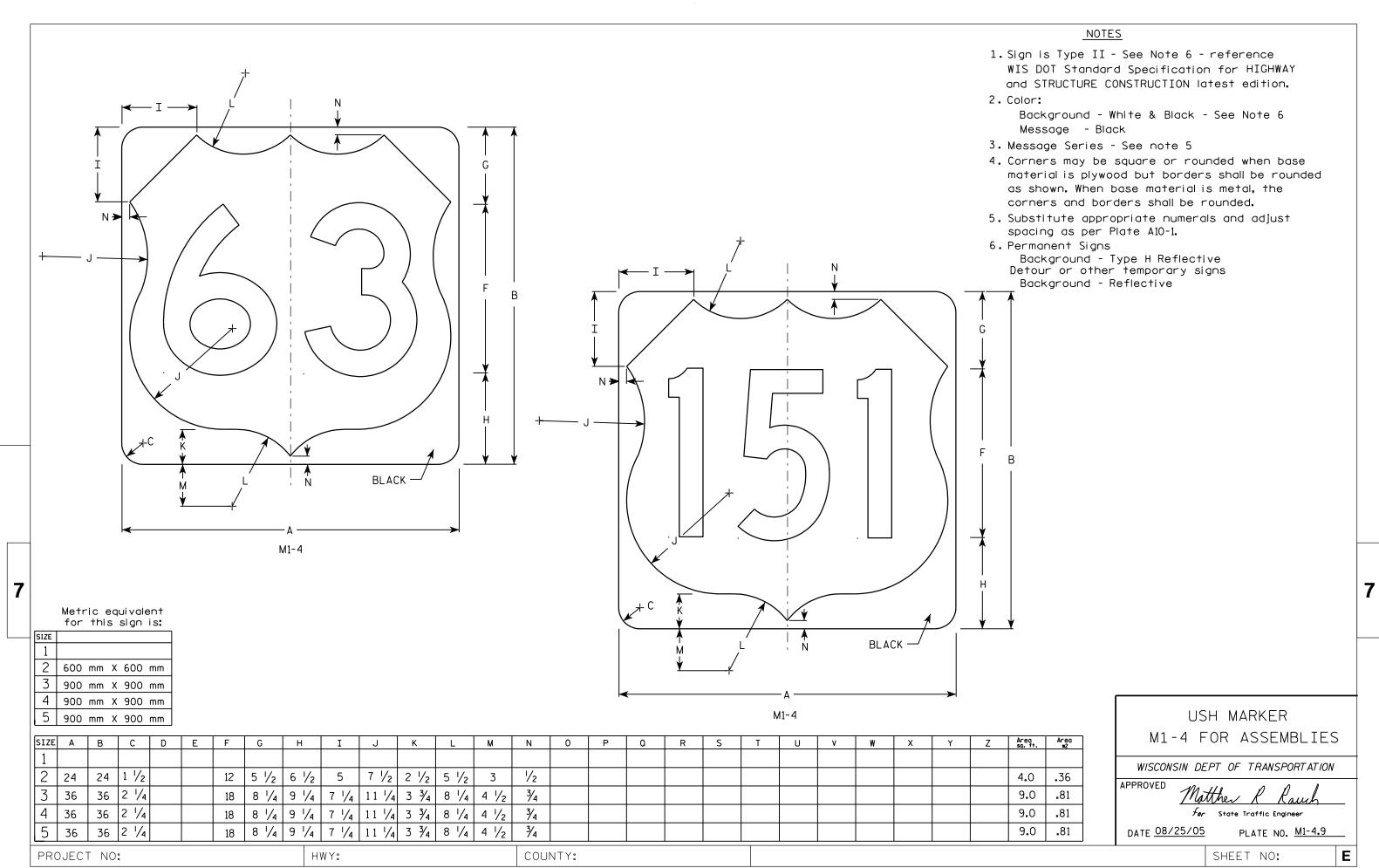
SHEET NO:

DATE 3/14/17

PLATE NO. G20-51.2

PLOT SCALE: 6.904489:1.000000

HWY:



FILE NAME : C:\Users\Projects\tr_stdplate\M14.DGN







MP3-1









HWY:



NOTES

- 1. All Signs Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

5. M3-1 thru M3-4 Background - White Message - Black

MB3-1 thru MB3-4 Background - Blue

Message - White

MK3-1 thru MK3-4 Background - Green

Message - White

MM3-1 thru MM3-4 Background - White

Message - Green

MN3-1 thru MN3-4 Background - Brown

Message - White

MP3-1 thru MP3-4 Background - White

Message - Blue

6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 1/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

COUNTY:

STANDARD SIGNS M3-1 thur M3-4 **SERIES**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 10/15/15 PLATE NO. M3-1.14

Ε

SHEET NO:

FILE NAME · C·\CAFfiles\Projects\tr stdnlote\M31 DCN

PROJECT NO:

PLOT DATE . 01-DEC-2015 17:54

PLOT RY . \$\$ plotuser \$\$ PLOT NAME :

PLOT SCALE . 11 675051.1 000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

) A G	
	;
→ G →	
Y	

Α С E F G H I J S Х Z D 0 10 10 1/4 1 1/8 3/8 3/8 24 2.0 3 36 1 1/8 3/8 1/2 4 1/2 14 5/8 14 1/2 4.5 4 5

COUNTY:

STANDARD SIGN M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 11/10/10 PLATE NO. M4-8.2

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M48.DGN

PROJECT NO:

HWY:

PLOT DATE: 10-NOV-2010 13:18

PLOT BY : ditjph

PLOT SCALE : 4.767

PLOT NAME :

PLOT SCALE: 4.767233:1.000000

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

2. Color:

Background - Orange Message - Black

- 3. Message Series B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

 $D \longrightarrow$ Н M4-8A

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	U	٧	w	Х	Y	Z	Area sq. ft.
$\parallel 1 \parallel$																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5				·	·						·				·												

COUNTY:

STANDARD SIGN M4-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther

For State Traffic Engineer DATE 3/9/11

PLATE NO. M4-8A.2

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M48A.DGN

HWY:

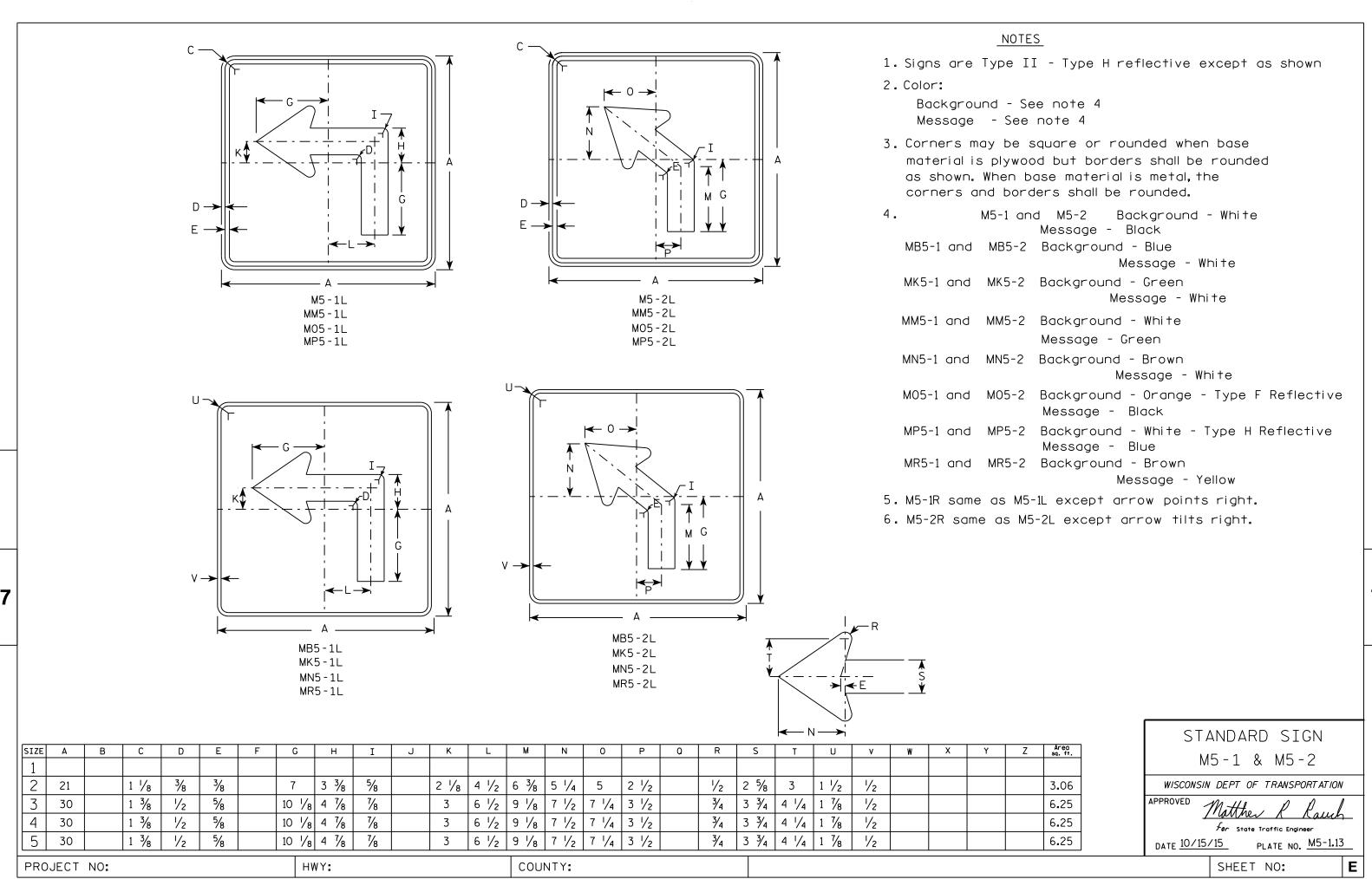
PROJECT NO:

PLOT DATE: 09-MAR-2011 10:29

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: 3.972696:1.000000



FILE NAME . C.\CAFfiles\Projects\tr stdolote\M51 DCN

PLOT DATE . 01-DEC-2015 18:07

PINT RY . \$\$ DIOTUSET \$\$ PINT NAMF :

PLOT SCALE . 11 675051.1 000000







MR6-1

HWY:



NOTES

- 1. Signs are Type II Type H except as Shown
- 2. Color:

Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background White

Message - Black

MB6-1 and MB6-2 Background - Blue

Message - White

MK6-1 and MK6-2 Background - Green

Message - White

MM6-1 and MM6-2 Background - White

Message - Green

MN6-1 and MN6-2 Background - Brown

Message - White

M06-1 and M06-2 Background - Orange - Type F Reflective

Message - Black

MP6-1 and MP6-2 Background - White

Message - Blue

MR6-1 and MR6-2 Background - Brown

Message - Yellow



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	٥	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1 1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 %	5	4 1/4	5 1/4	3	2 %	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew & Rawl For State Traffic Engineer

Ε

DATE 10/15/15 PLATE NO. M6-1.15

SHEET NO:

FILE NAME · C·\CAFfiles\Projects\tr stdplote\M61 DCN

PROJECT NO:

PLOT DATE . 01-DEC-2015 17:57

PIOT RY . \$\$ plotuser \$\$ PIOT NAMF :

PLOT SCALE . 11 675051.1 000000

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

C —		\
D A E A		$ \begin{array}{c c} G & \hline & F & \hline & B & \hline & G & G & G & \hline & G & G & G & G & \hline & G & G & G & G & \hline & G & G & G & G & G & G \\ & G & G & G & G & G & G $
	R11-2B	

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	M	N	0	Р	0	R	S	T	U	V	W	X	Y	Z	Areg sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
2M	48	30	1 %	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
4	48	30	1 %	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	19 ¾	9 3/4	9 %																10.0

STANDARD SIGN R11-2B

WISCONSIN DEPT OF TRANSPORTATION

DATE 4/1/11 PLATE NO. R11-2B.2

SHEET NO:



- 1. Sign is Type II Type H Reflective
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.

R11-3 ** See Note 5

SIZE A В С D Е G 5/8 1 3/8 1/2 1 1/8 | 15 1/4 | 8 10 3/4 8 3/8 4 3/4 6 3/4 36 18 4 3 2 1/2 2 2 11 1/8 6 1/2 2 7 1/8 4.5 1/2 17 3/8 13 1/8 30 $1\frac{3}{8}$ 5/8 4 1/4 3 3/8 16 5/8 1 1/2 23 | 13 1/4 | 1 3/4 3 1/2 11 1/8 12.5 6 10 11 2M 4 1/4 3 3/8 16 5/8 1 1/2 23 | 13 1/4 | 1 3/4 30 17 3/8 13 1/8 10 3 1/2 12.5 3 4 5

COUNTY:

STANDARD SIGN R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R Rauch DATE 3/15/17 PLATE NO. R11-3.8

SHEET NO:

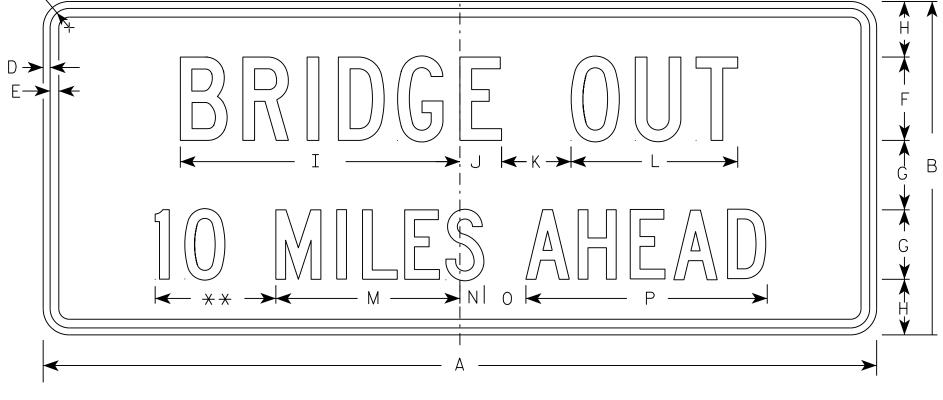
HWY:



- 1. Sign is Type II Type H Reflective
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3C

** See Note 5

1/4 MILF AH

SIZE	Α	В	С	D	E	F	G	Н	I	٦	K	L	М	N	0	Р	Q	R	S	Т	C	٧	W	Х	Υ	Z	Area sq. ft.
1	36	15	1 3/8	1/2	5/8	4	3	2 1/2	13 1/4	2 1/4	3	8	8	1 1/2	2	10 ¾		7 1/8									3.75
2S	60	24	1 3/8	1/2	5/8	6	5	4	20 1/8	3	5	12	13 1/4	1 3/4	3	17 3/8		11 1/8									10.0
2M	60	24	1 3/8	1/2	5/8	6	5	4	20 1/8	3	5	12	13 1/4	1 3/4	3	17 3/8		11 1/8									10.0
3																											
4																											
5																											

STANDARD SIGN R11-3C

WISCONSIN DEPT OF TRANSPORTATION

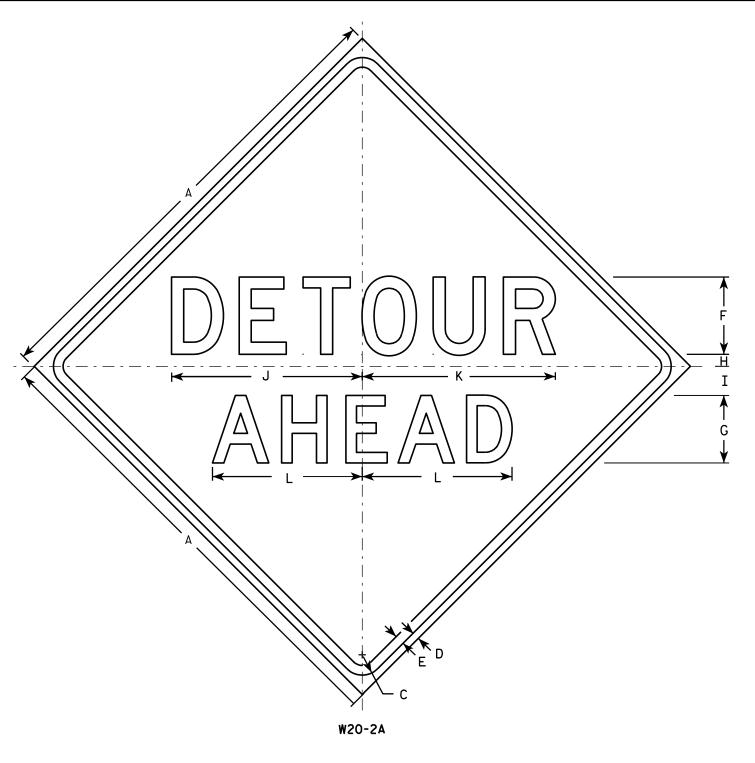
APPROVED

Matther R Rauch
For State Traffic Engineer

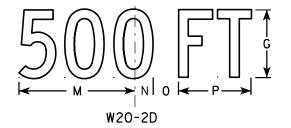
DATE <u>7/28/16</u>

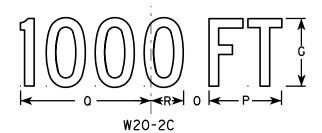
PLATE NO. R11-3C.3

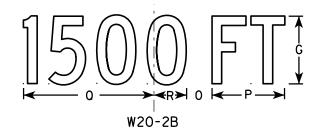
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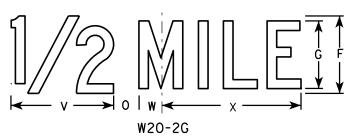


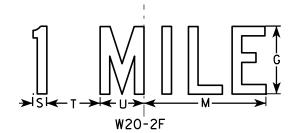
HWY:











PLOT BY: mscj9h

<u>NOTES</u>

- Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Line 1 is Series D.
 Line 2 is Series D for AHEAD and
 Series C for all other distances.

SIZE	Α	В	С	D	Ε	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	U	v	W	X	Y	Z	Areo sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 1/8	5 %	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
3	48		2 1/4	¾	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 %	2 %	7 1/2	13 ½	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
4	48		2 1/4	¾	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 %	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 %	2 3/8	14 3/8			16.0

COUNTY:

STANDARD SIGN W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

DATE 3/18/11 PLATE NO. W20-2.6

SHEET NO:

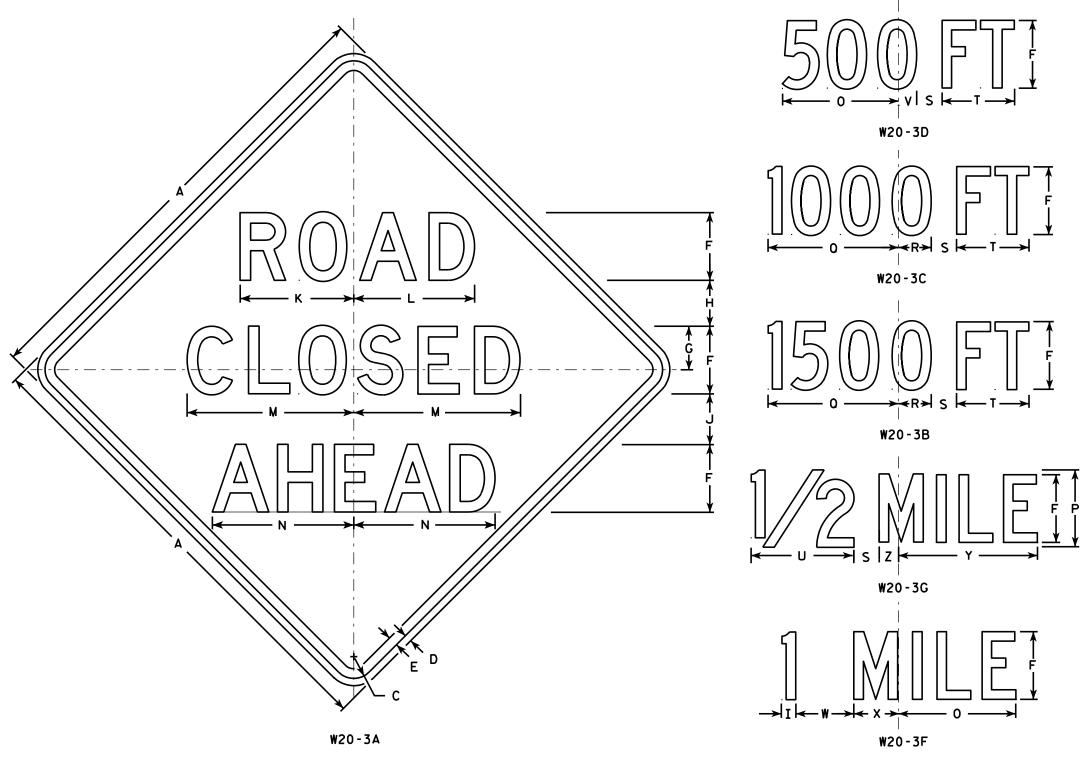
PROJECT NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W202.DGN

PLOT DATE: 18-MAR-2011 10:00

PLOT NAME :

PLOT SCALE: 9.931739:1.000000



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series see note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Lines 1 and 2 are Series D. Line 3 is Series D for AHEAD and Series C for all other distances.

1 % 5/8 ¾ 8 3/8 8 7/8 12 1/2 5 % 1 3/8 4 1/2 36 3 1/2 10 3/4 1 3/4 8 4 \(\frac{5}{8} \) 14 \(\frac{3}{8} \) 2 \(\frac{3}{8} \) 16.0 3/4 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 5/8 1 7/8 2M 3/4 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 48 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 % 1 % 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 3/4 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 4 % | 14 % | 2 % | 16.0 48 3/4 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 13 1/2 3 3/8 2 5/8 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 7 1/2 10 5/8 1 7/8 48 5 4 5/8 14 3/8 2 3/8 16.0 3/4 2 1/4 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 48

COUNTY:

STANDARD SIGN W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer DATE 3/18/11

PLATE NO. W20-3.7

SHEET NO:

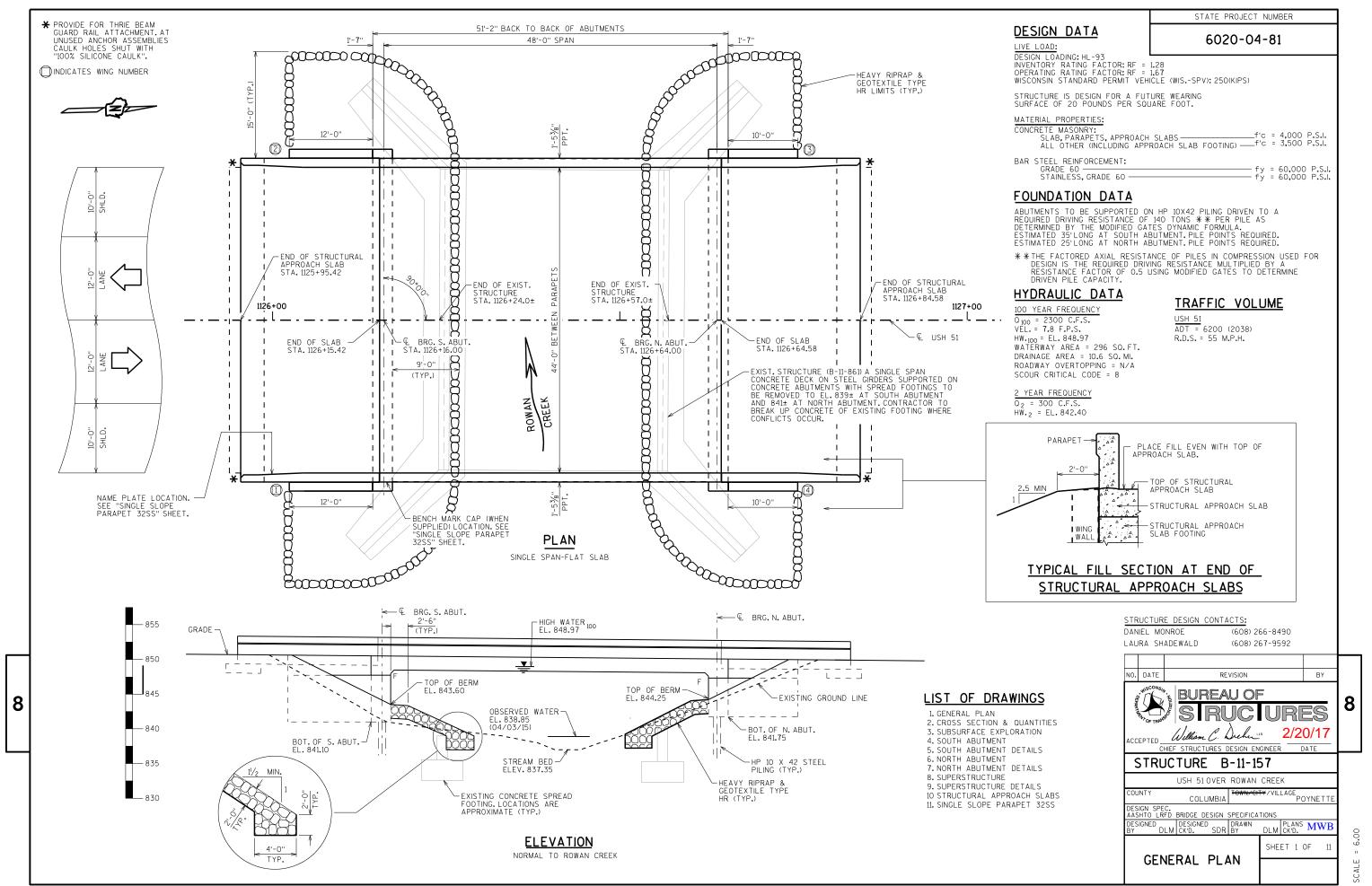
PROJECT NO: FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN HWY:

PLOT DATE: 18-MAR-2011 12:08

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: 9.931739:1.000000



STATE PROJECT NUMBER

6020-04-81

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS OTHERWISE NOTED.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL. ALSO EXCLUDED IS THE "BASE AGGREGATE DENSE 1 1/4-INCH" AS DETAILED ON THE STRUCTURAL APPROACH SLAB SHEET.

THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

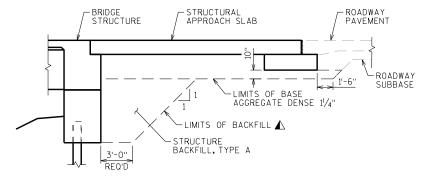
PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE TOP OF DECK AND APPROACH SLAB SURFACES.

PIGMENTED PROTECTIVE SURFACE SEALER TO BE APPLIED TO THE FRONT FACE AND THE TOP OF THE PARAPETS, INCLUDING PARAPETS ON APPROACH SLABS.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE "HR" TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS.

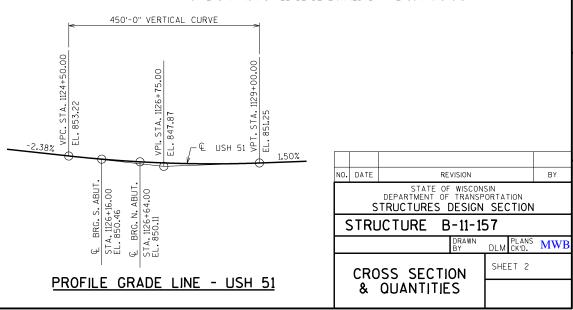
SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE, UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

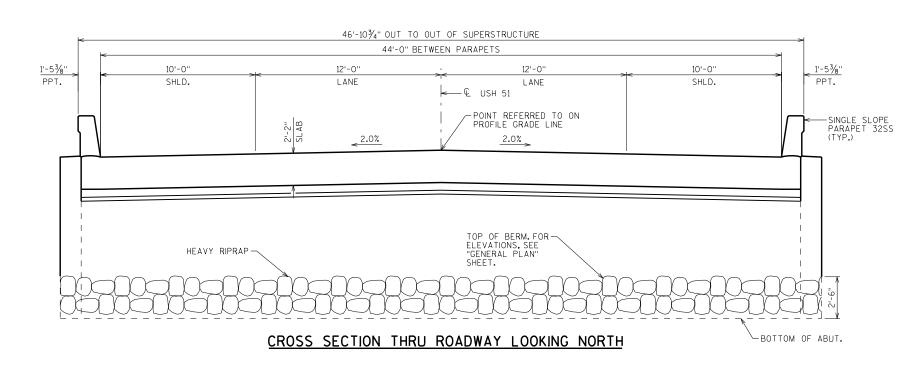
SAWCUT AND REMOVE EXISTING CONCRETE ABUTMENTS AND WINGS TO ELEVATION $839\pm$ AT SOUTH ABUTMENT AND $841\pm$ AT NORTH ABUTMENT. INCORPORATE THE EXISTING ABUTMENTS AND WINGS IN STREAM BANK PROTECTION AS SHOWN ON THE GENERAL PLAN SHEET.



TYPICAL SECTION THRU ABUTMENT

⚠ BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.





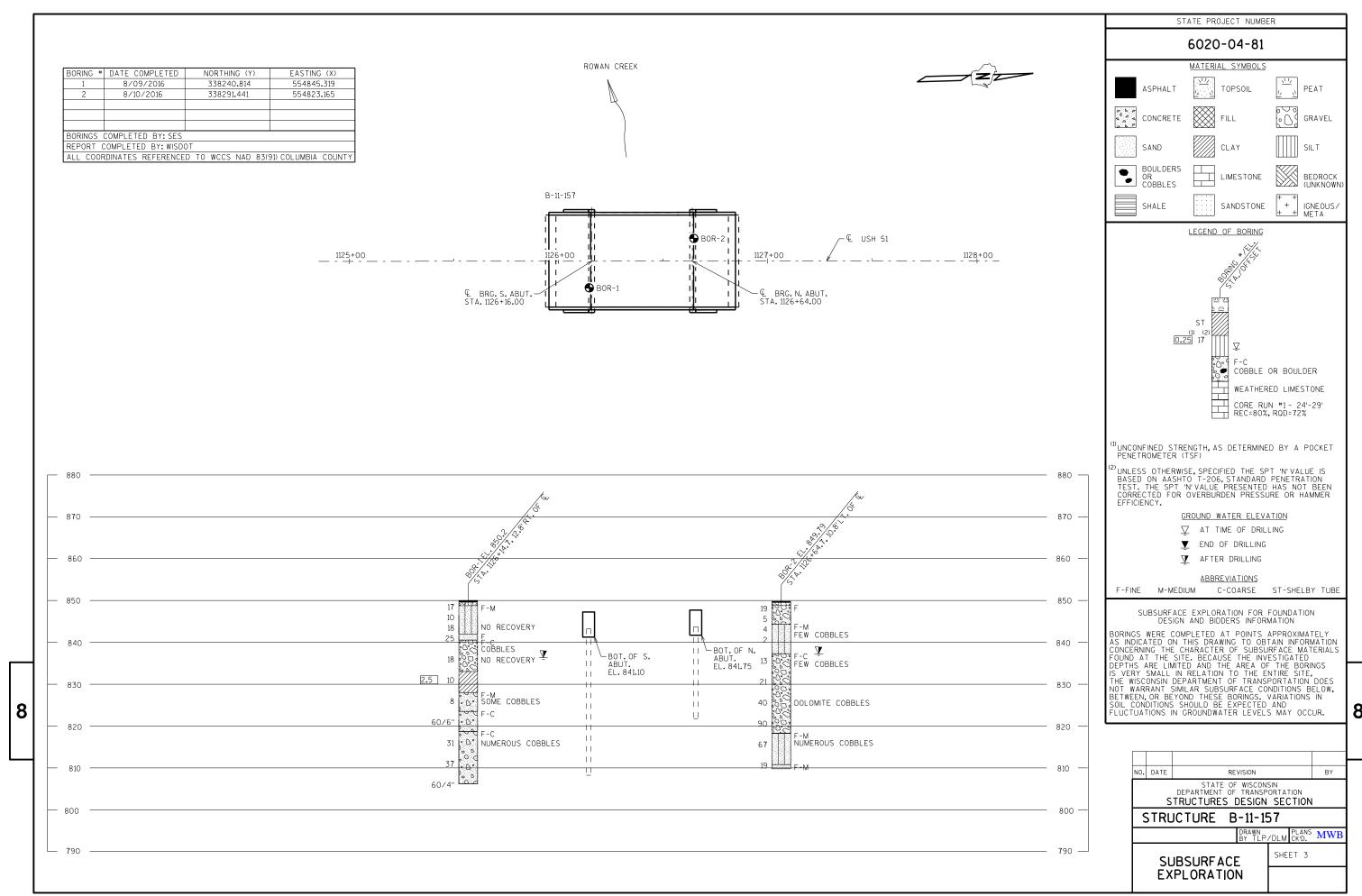
TOTAL ESTIMATED QUANITITES

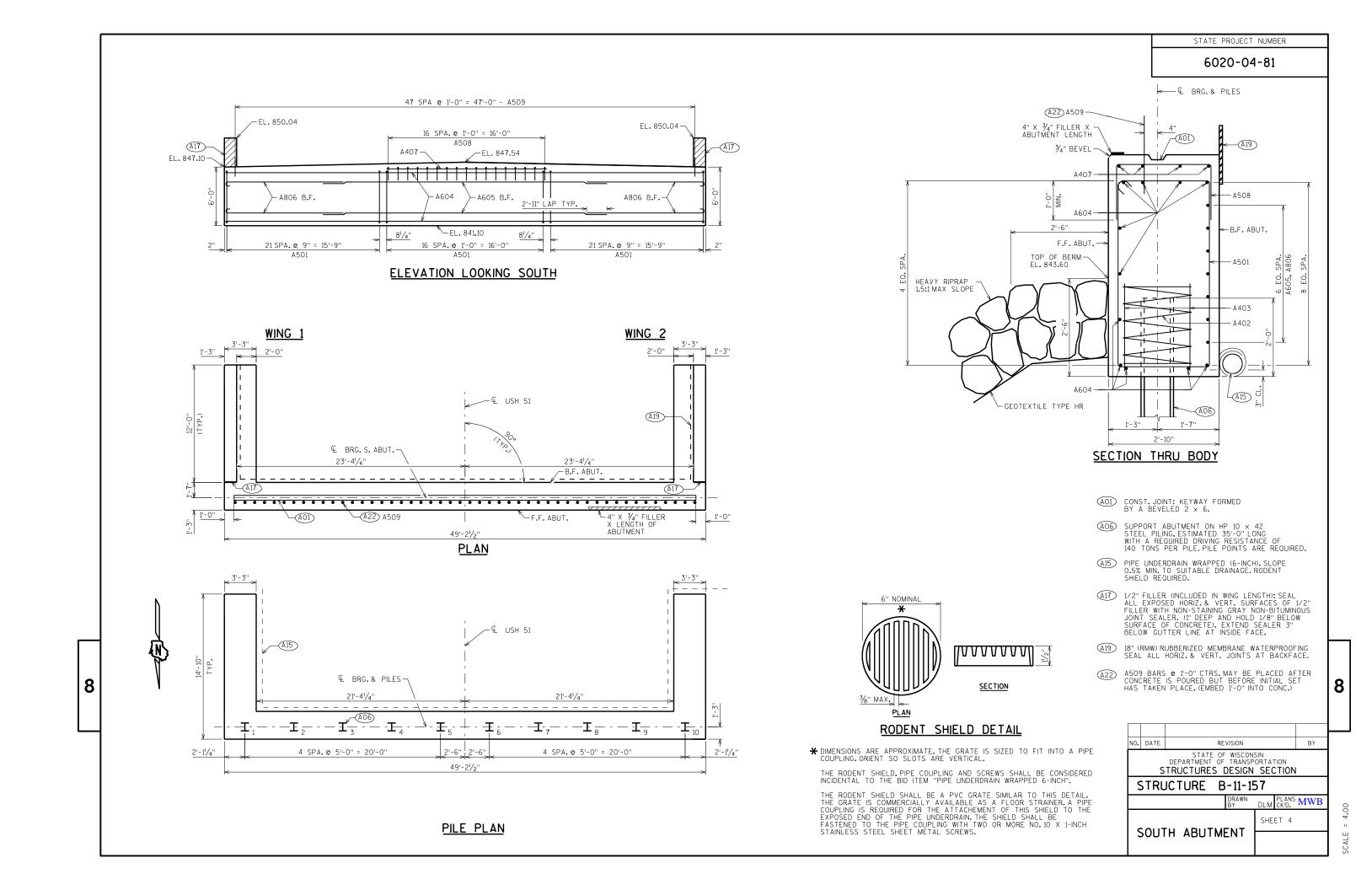
BID ITEM NUMBER	BID ITEMS	UNIT	SUPER	SOUTH APPROACH	SOUTH ABUT.	NORTH ABUT.	NORTH APPROACH	TOTALS
203.0700.5	REMOVING OLD STRUCTURE OVER WATERWAY WITH DEBRIS CAPTURE SYSTEM STA. 1126+24	LS						1
206.1000	EXCAVATION FOR STRUCTURES BRIDGES B-11-157	LS						1
210.1500	BACKFILL STRUCTURE TYPE A	TON			115	86		201
305.0120	BASE AGGREGATE DENSE 1-1/4"	TON		147			147	294
502.0100	CONCRETE MASONRY BRIDGES	CY	205	64	53	42	64	428
502.3200	PROTECTIVE SURFACE TREATEMENT	SY	241	110			110	461
502,3210	PIGMENTED SURFACE SEALER	SY	41	17			17	75
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB			3,140	2,970		6,110
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	35,080	10,700	1,610	1,230	10,700	59,320
505.0800.S	BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES	LB			220	220		440
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY			12	12		24
550.0500	PILE POINTS	EACH			10	10		20
550.1100	PILING STEEL HP 10-INCH X 42LB	LF			350	250		600
606.0300	RIPRAP HEAVY	CY			105	100		205
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF			95	90		185
614.0150	ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD	EACH		2			2	4
645.0120	GEOTEXTILE TYPE HR	SY			175	170		345
	NON-BID ITEMS							
	FILLER							1/2", 3/4"&11/2"

8

CALF = 3.00

8





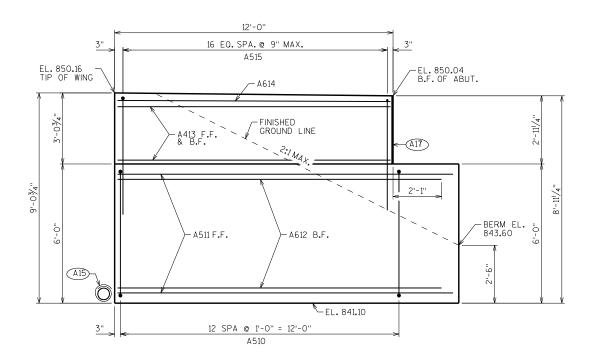
STATE PROJECT NUMBER

6020-04-81

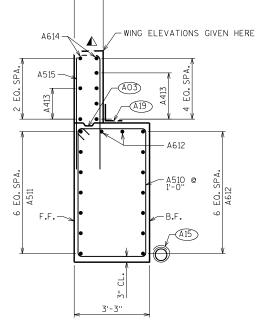
BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

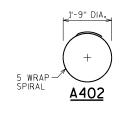
BAR MARK	180°	NO. REQ'D.	LENGTH	SEN S	BAR SERIES	LOCATION
A501		61	16'-10"	Х		BODY-STIRRUP
A402		10	28'-0"	Х		BODY-1 PER BODY PILE-VERT.
A403		20	2'-3"			BODY-2 PER BODY PILE-VERT.
A604		11	48'-10"			BODY-HORIZF.F.
A605		7	30'-9"			BODY-HORIZB.F.
A806		14	12'-11"	Х		BODY-HORIZB.F.
A407		4	16'-4"			BOD-HORIZTOP
A508		17	5'-3"	Х		BODY-VERTTOP
A509		48	2'-0"			BODY-VERTDOWELS
A510	Х	26	17'-8''	Х		WINGS-STIRRUP
A511	Х	14	14'-6"			WINGS 1 & 2-HORIZF.F.
A612	Χ	18	13'-11''			WINGS 1 & 2-HORIZB.F.
A413	Χ	12	11'-8''			WINGS 1 &2-HORIZ.
A614	Х	4	11'-8''			WINGS 1&2-HORIZ.
A515	Х	34	10'-8"	Х		WINGS 1&2-VERT.

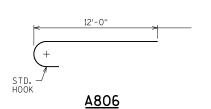


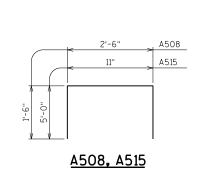
WING 1 AND 2 ELEVATION (F.F. WING 1 SHOWN, WING 2 SIMILAR)

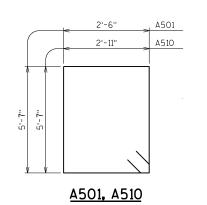


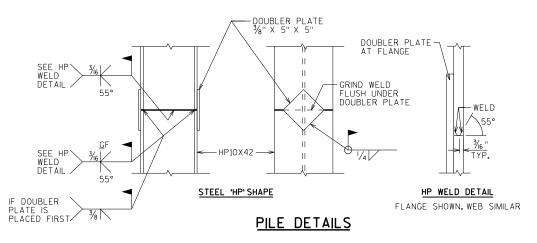
WING 1 AND 2 SECTION











▲ SLOPE TO DRAIN

- (AO3) OPTIONAL CONST. JOINT: KEYWAY FORMED BY BEVELED 2 x 6. (18" RMW @ B.F. & 3/4" "V" GROOVE @ F.F. IF JOINT IS USED).
- A15 PIPE UNDERDRAIN WRAPPED (6-INCH), SLOPE 0.5% MIN. TO SUITABLE DRAINAGE, RODENT SHIELD REQUIRED.
- A17

 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- (A19) 18" (RMW) RUBBERIZED MEMBRANE WATERPROOFING SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.

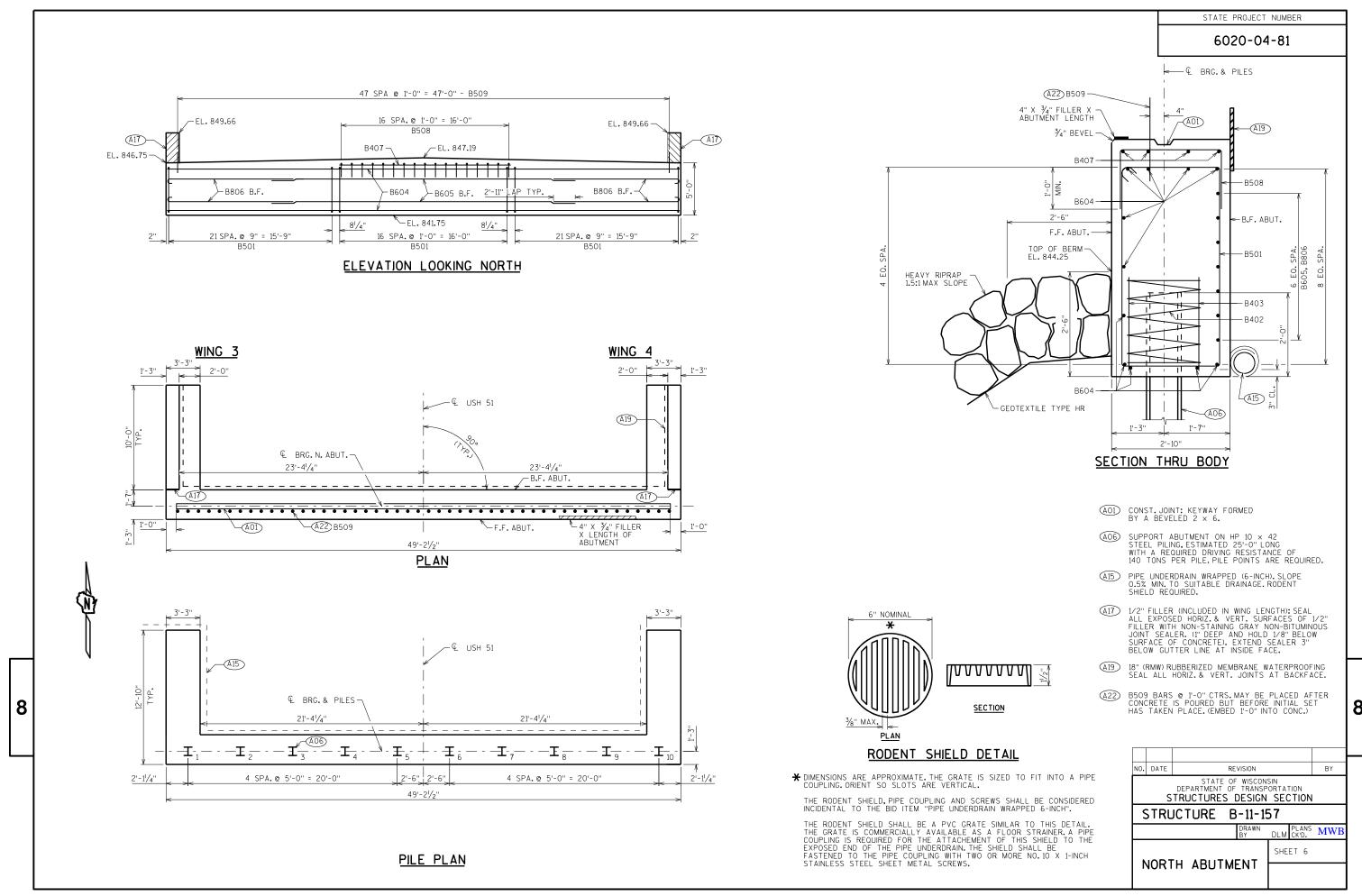
NO. DATE BY REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-11-157

DLM PLANS MWB SHEET 5

SOUTH ABUTMENT DETAILS

8



CALE = 4.00

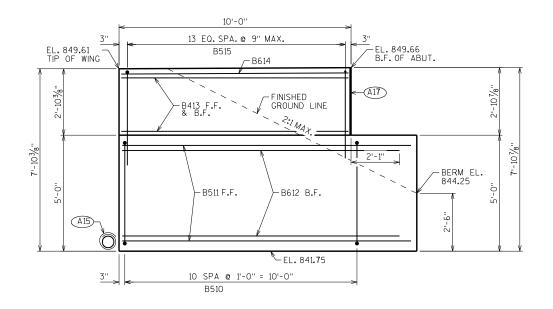
STATE PROJECT NUMBER

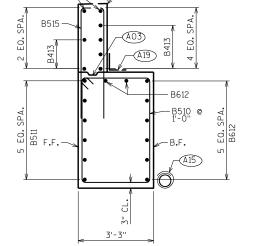
6020-04-81

BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

BAR MARK	1800 1800	NO. REQ'D.	LENGTH	BEN	BAR SERIES	LOCATION
B501		61	14'-8''	Х		BODY-STIRRUP
B402		10	28'-0"	Х		BODY-1 PER BODY PILE-VERT.
B403		20	2'-3"			BODY-2 PER BODY PILE-VERT.
B604		11	48'-10"			BODY-HORIZF.F.
B605		7	34'-9"			BODY-HORIZB.F.
B806		14	10'-11"	Х		BODY-HORIZB.F.
B40 7		4	16'-4''			BOD-HORIZTOP
B508		17	5'-3"	Х		BODY-VERTTOP
B509		48	2'-0"			BODY-VERTDOWELS
B510	Х	22	15'-6"	Х		WINGS 3&4-STIRRUP
B511	Х	12	12'-6"			WINGS 3&4-HORIZF.F.
B612	Х	16	11'-11''			WINGS 3&4-HORIZB.F.
B413	Х	12	9'-8"			WINGS 3&4-HORIZ.
B614	Х	4	9'-8"			WINGS 3&4-HORIZ.
B515	Х	28	10'-2"	Х		WINGS 3&4-VERT.





- WING ELEVATIONS GIVEN HERE

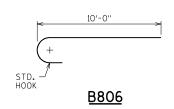
WING 3 AND 4 ELEVATION

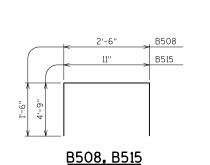
(F.F. WING 3 SHOWN, WING 4 SIMILAR)

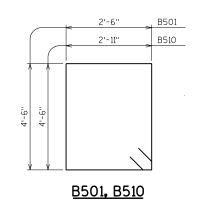
WING 3 AND 4 SECTION

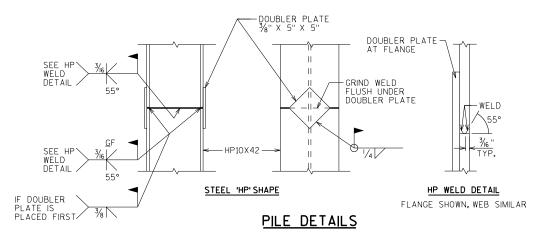


8









⚠ SLOPE TO DRAIN

- (AO3) OPTIONAL CONST. JOINT: KEYWAY FORMED BY BEVELED 2 x 6. (18" RMW @ B.F. & 3/4" "V" GROOVE @ F.F. IF JOINT IS USED).
- A15 PIPE UNDERDRAIN WRAPPED (6-INCH), SLOPE 0.5% MIN. TO SUITABLE DRAINAGE, RODENT SHIELD REQUIRED.
- A17

 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
- (A19) 18" (RMW) RUBBERIZED MEMBRANE WATERPROOFING SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.

NO. DATE REVISION BY

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION

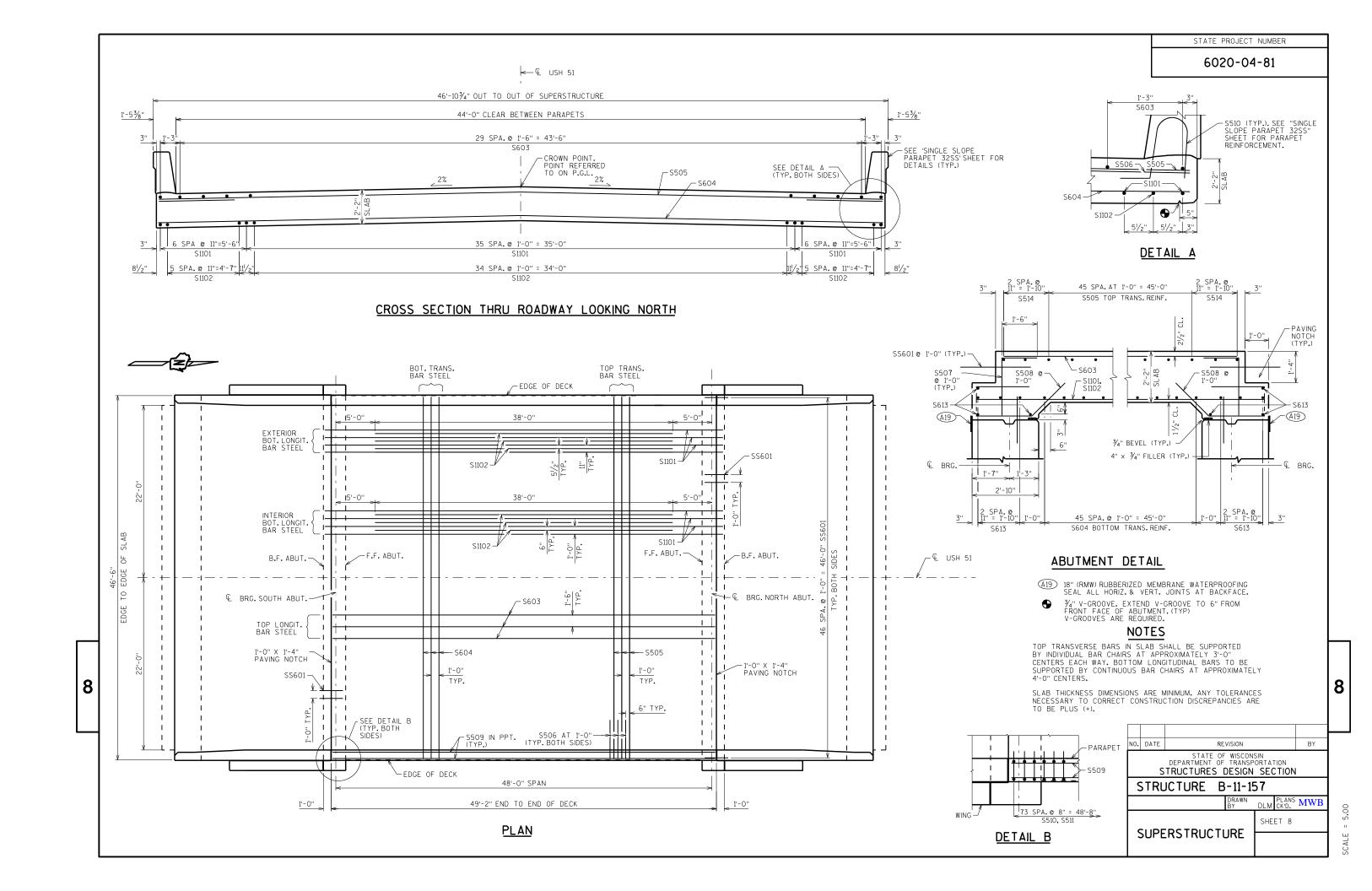
STRUCTURE B-11-157

DRAWN DLM CKD. MWB

SHEET 7

NORTH ABUTMENT DETAILS

8



6020-04-81

SURVEY TOP OF SLAB ELEVATIONS

	SOUTH ABUTMENT	5/10 PT.	NORTH ABUTMENT
W. GUTTER			
€ USH 51			
E. GUTTER			

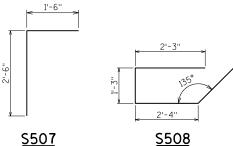
PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF DECK ELEVATIONS AT THE ${\mathbb Q}$ OF ABUTMENTS, THE ${\mathbb Q}$ OF PIERS AND AT 5/10 PTS. TO VERIFY CAMBER, TAKE ELEVATIONS ALONG GUTTER LINES AND CROWN OR ${\mathbb R}$. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

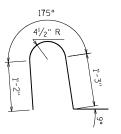
BILL OF BARS

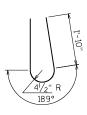
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE

	BAR MARK	200	NO. REQ'D.	LENGTH	SUN	BAR SERIES	LOCATION				
ı	S1101	Х	48	50'-10"			SLAB-LONGITBOT				
	S1102	Х	47	38'-0"			SLAB-LONGITBOT				
	S603	Х	32	48'-10"			SLAB-LONGITTOP				
	S604	Х	46	46'-2"			SLAB-TRANSVERSE-BOTIN SPAN				
	S505	Х	46	46'-2"			SLAB-TRANSVERSE-TOP-IN SPAN				
	S506	Х	98	5'-0"			SLAB-TRANSVERSE-TOP-EDGE OF SLAB				
	S50 7	Х	94	3'-11''	Х		SLAB AT ABUTMENTS				
	S508	08 X 94 7 '-6'' X			SLAB AT ABUTMENTS						
	S509 X		12	48'-10"			32SS PARAPET-HORIZ.				
	S510	Х	148	4'-5"	Х		32SS PARAPET - VERT.				
	S511	Х	148	5'-0"	Х		32SS PARAPET-VERT.				
ĺ	S512				SLAB AT ABUTMENTS-ENDS						
	S613				SLAB-TRANSVERSE-BOTAT ABUT.						
	S514	Х	4	48'-9"			SLAB-TRANSVERSE-TOP-AT ABUT.				
	S415	Х	8	2'-6"			SLAB AT ABUTMENTS-VERTENDS				
ı	S416	Χ	4	0"-10"			SLAB AT ABUTMENTS-HORIZENDS				
>	SS601	S	94	3'-0"			STRUCTURE SLAB TO APPROACH SLAB				

-STAINLESS STEEL







<u>S510</u>

<u>S511</u>

<u>S512</u>

NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

STRUCTURE B-11-157

DLM PLANS MWB SHEET 9 SUPERSTRUCTURE

DETAILS

←SLAB THICKNESS CAMBER AND SLAB THICKNESS DIAGRAM CAMBER SHOWN IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. PARAPETS, SIDEWALKS AND MEDIANS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED.

<--CAMBER

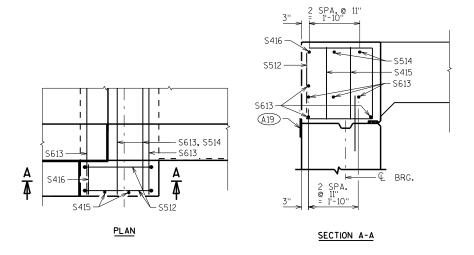
48'-0"

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

TOP OF SLAB ELEVATION AT FINAL GRADE SLAB THICKNESS CAMBER

FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)

EQUALS TOP OF SLAB FALSEWORK ELEVATION.



ABUTMENT END DETAIL

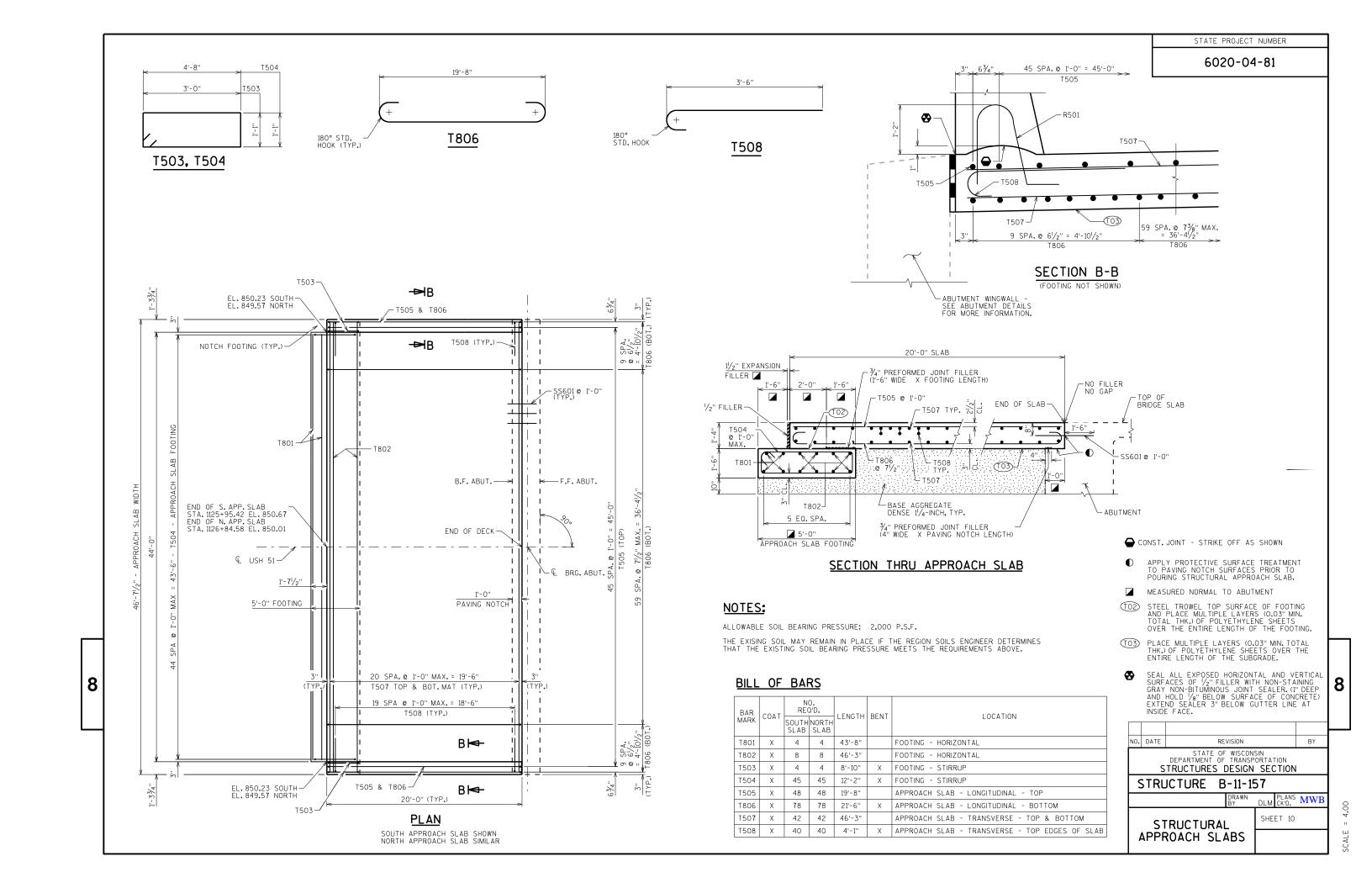
(TYP. ALL CORNERS)

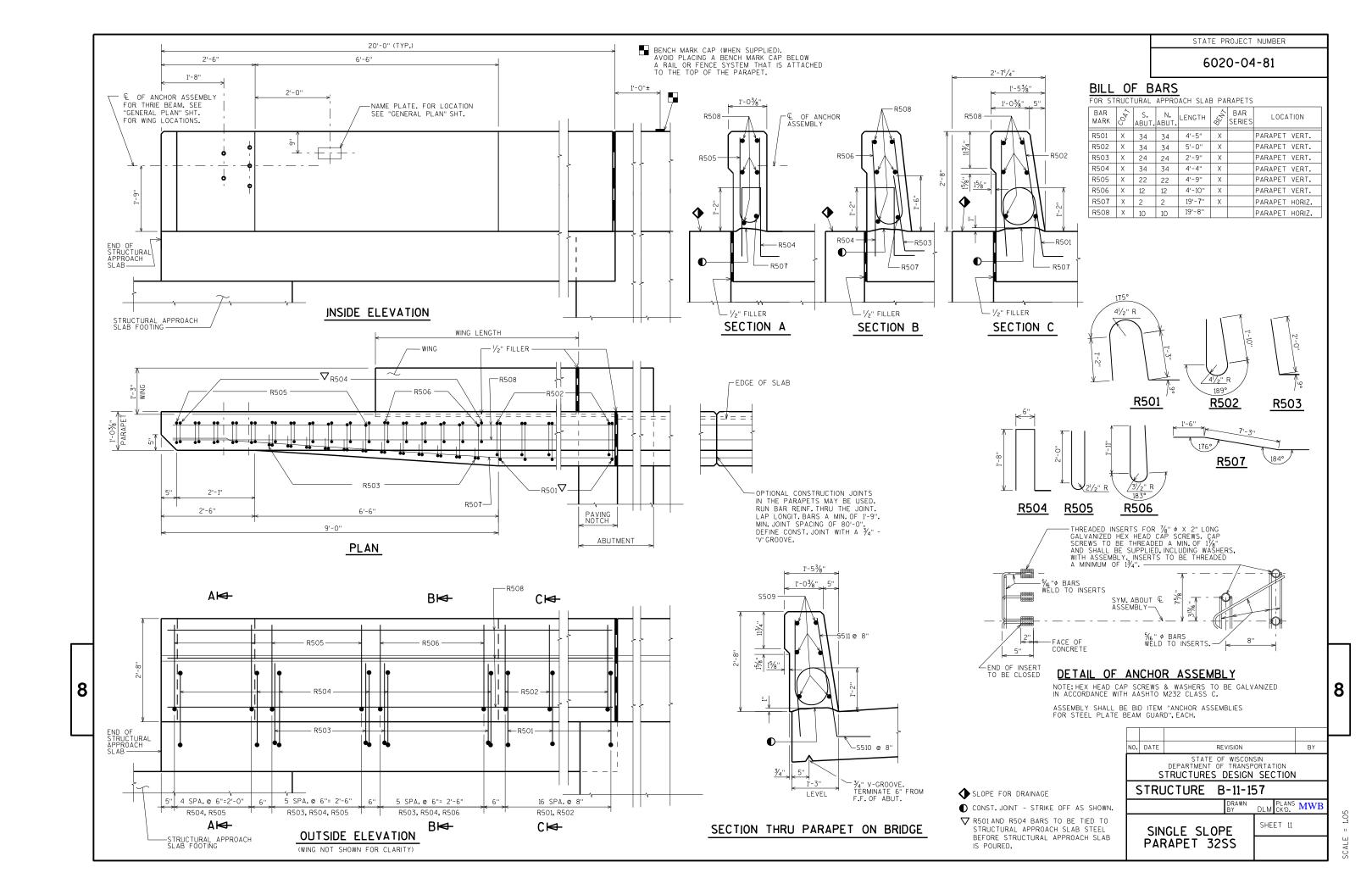
TOP OF DECK ELEVATIONS

	€ BRG. S. ABUT.	1/10	2/10	3/10	4/10	5/10	6/10	7/10	8/10	9/10	€ BRG. N. ABUT.
W. EDGE OF DECK	850.02	849.98	849.93	849.89	849.85	849.82	849.78	849.75	849.72	849.69	849.67
CROWN	850.46	850.42	850.37	850.33	850.29	850.26	850.22	850.19	850.16	850.13	850.11
E.EDGE OF DECK	850.02	849.98	849.93	849.89	849.85	849.82	849.78	849.75	849.72	849.69	849.67

8

BY





EARTHWORKTABLE

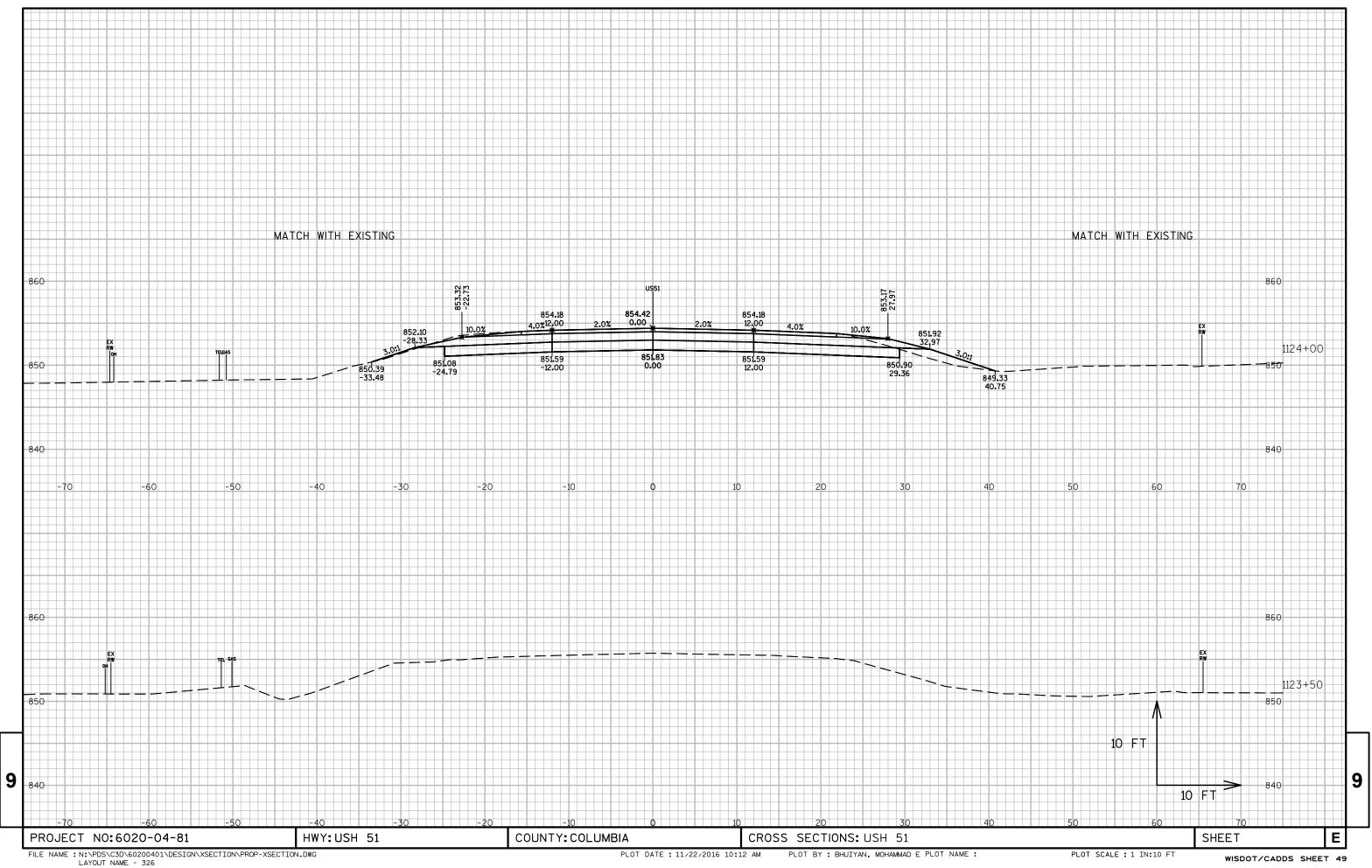
	AREA (SF)			Incrementa	l Vol (CY) (Unadju	Cumulative Vol (CY)				
				Salvaged/Unusable					Expanded	
			Cut	Pavement Material	Fill	Cut	Fill	Cut	Fill	Mass Ordinate
STATION	Real Station	Distance						1.00	1.25	
						Note 1	Note 3	Note 1		Note 8
1122+00	112200	0.00	0.00	0.00	0.00	0	0	0	0	0
1122+50	112250	50.00	0.00	0.00	0.00	0	0	0	0	0
1123+00	112300	50.00	0.00	0.00	0.00	0	0	0	0	0
1123+25	112325	25.00	0.00	0.00	0.00	0	0	0	0	0
1123+50	112350	25.00	0.00	0.00	0.00	0	0	0	0	0
1124+00	112400	50.00	134.58	0.00	8.64	125	8	125	10	115
1124+28	112428	28.00	134.74	0.00	25.20	140	18	264	32	232
1124+50	112450	22.00	141.51	0.00	23.72	113	20	377	57	320
1124+53	112453	3.00	142.31	0.00	23.62	16	3	393	60	332
1124+78	112478	25.00	147.68	0.00	24.13	134	22	527	88	439
1125+00	112500	22.00	151.63	0.00	26.53	122	21	649	114	535
1125+02	112502	2.00	151.55	0.00	26.81	11	2	660	116	544
1125+27	112527	25.00	144.94	0.00	22.13	137	23	797	144	653
1125+50	112550	23.00	143.27	0.00	5.56	123	12	920	159	761
1125+52	112552	2.00	143.12	0.00	5.62	11	0	931	160	771
1126+00	112600	48.00	135.48	0.00	20.60	248	23	1,178	189	990
1126+50	112650	50.00	105.27	0.00	293.93	223	291	1,401	553	848
1126+75	112675	25.00	134.39	0.00	26.71	111	148	1,512	738	774
1127+00	112700	25.00	151.94	0.00	0.53	133	13	1,645	754	891
1127+12	112712	12.00	156.58	0.00	0.11	69	0	1,713	754	959
1127+37	112737	25.00	164.77	0.00	0.52	149	0	1,862	755	1,107
1127+50	112750	13.00	159.31	0.00	0.84	78	0	1,940	755	1,185
1127+62	112762	12.00	158.26	0.00	0.91	71	0	2,011	756	1,255
1128+00	112800	38.00	165.31	0.00	0.00	228	1	2,238	756	1,482
1128+04	112804	4.00	172.35	0.00	0.00	25	0	2,263	756	1,507
1128+10	112810	6.00	171.22	0.00	0.00	38	0	2,302	756	1,545
1128+29	112829	19.00	146.92	0.00	0.06	112	0	2,413	756	1,657
1128+50	112850	21.00	150.98	0.00	0.04	116	0	2,529	756	1,773
1128+54	112854	4.00	150.68	0.00	0.14	22	0	2,552	756	1,795
1129+00	112900	46.00	141.39	0.00	0.41	249	0	2,800	757	2,043
1129+50	112950	50.00	0.00	0.00	0.00	131	0	2,931	758	2,174

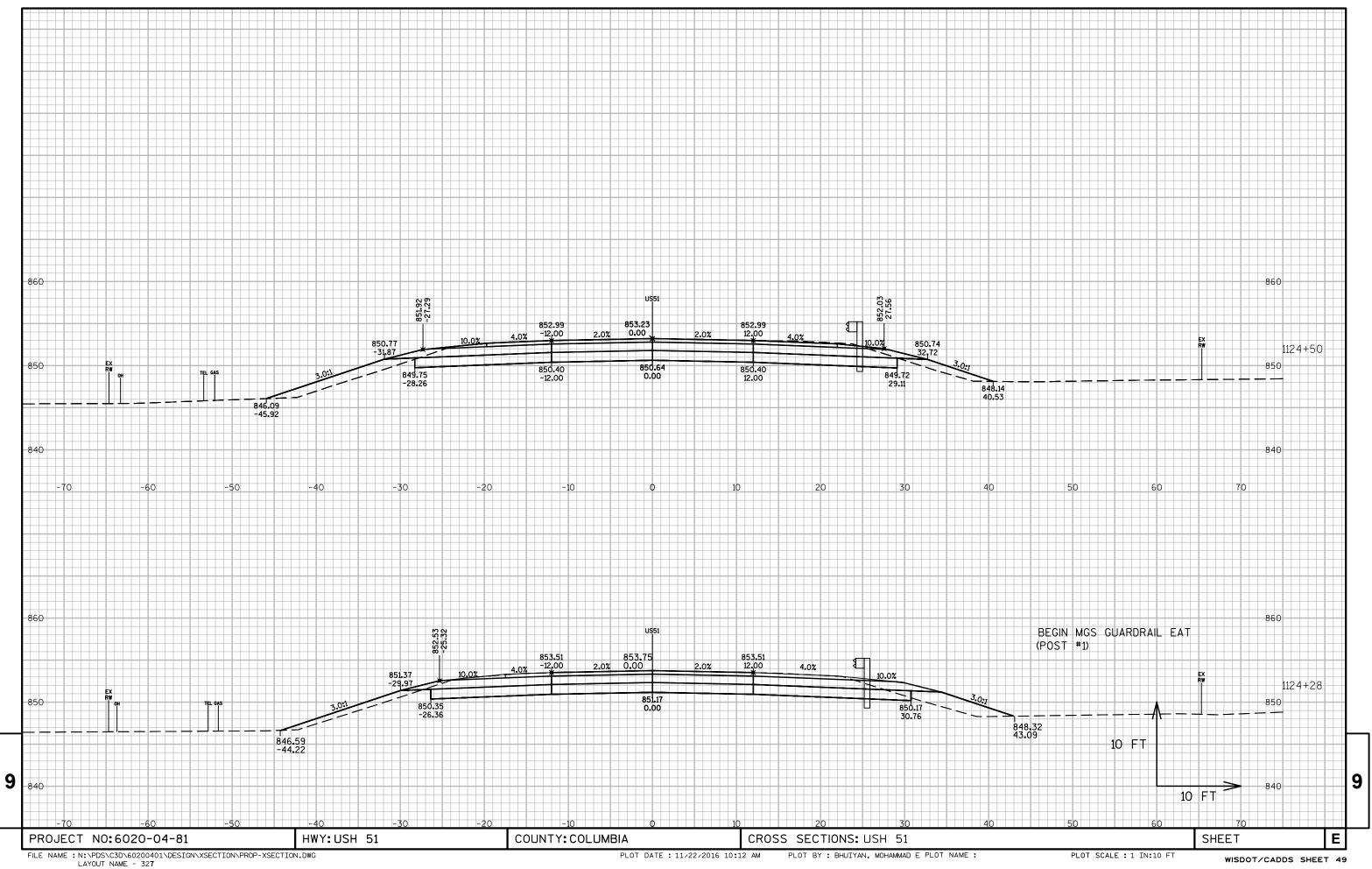
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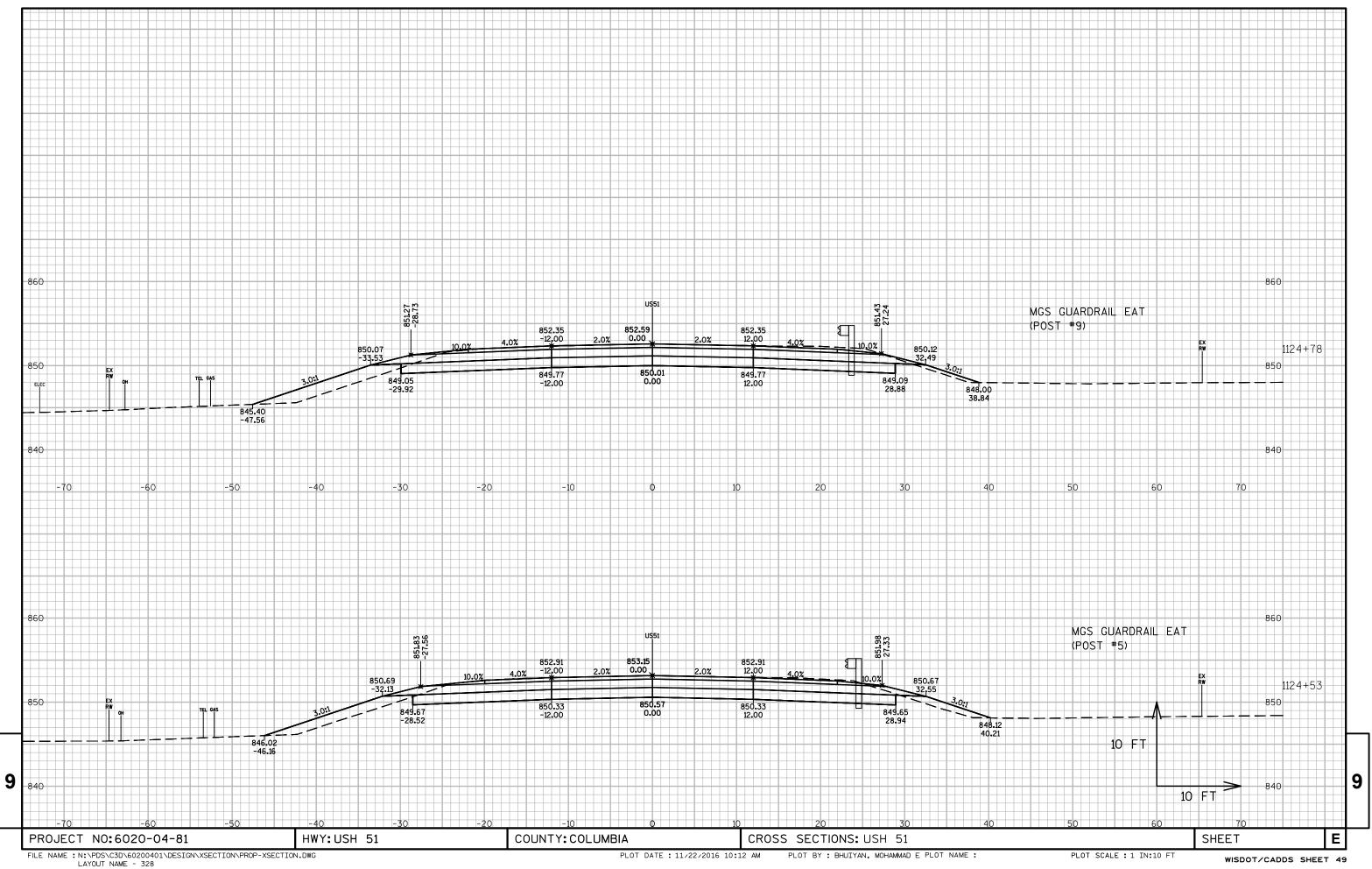
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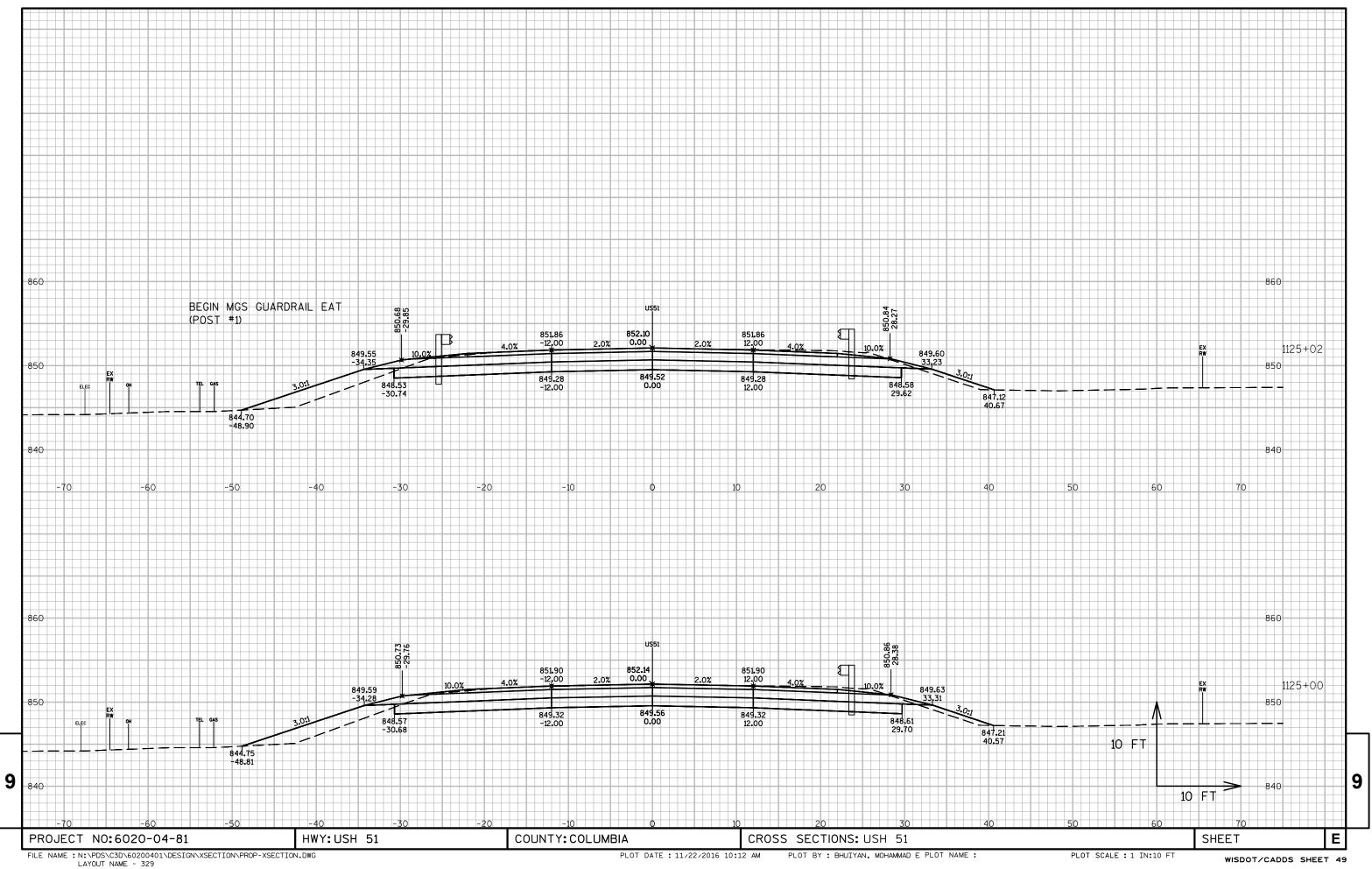
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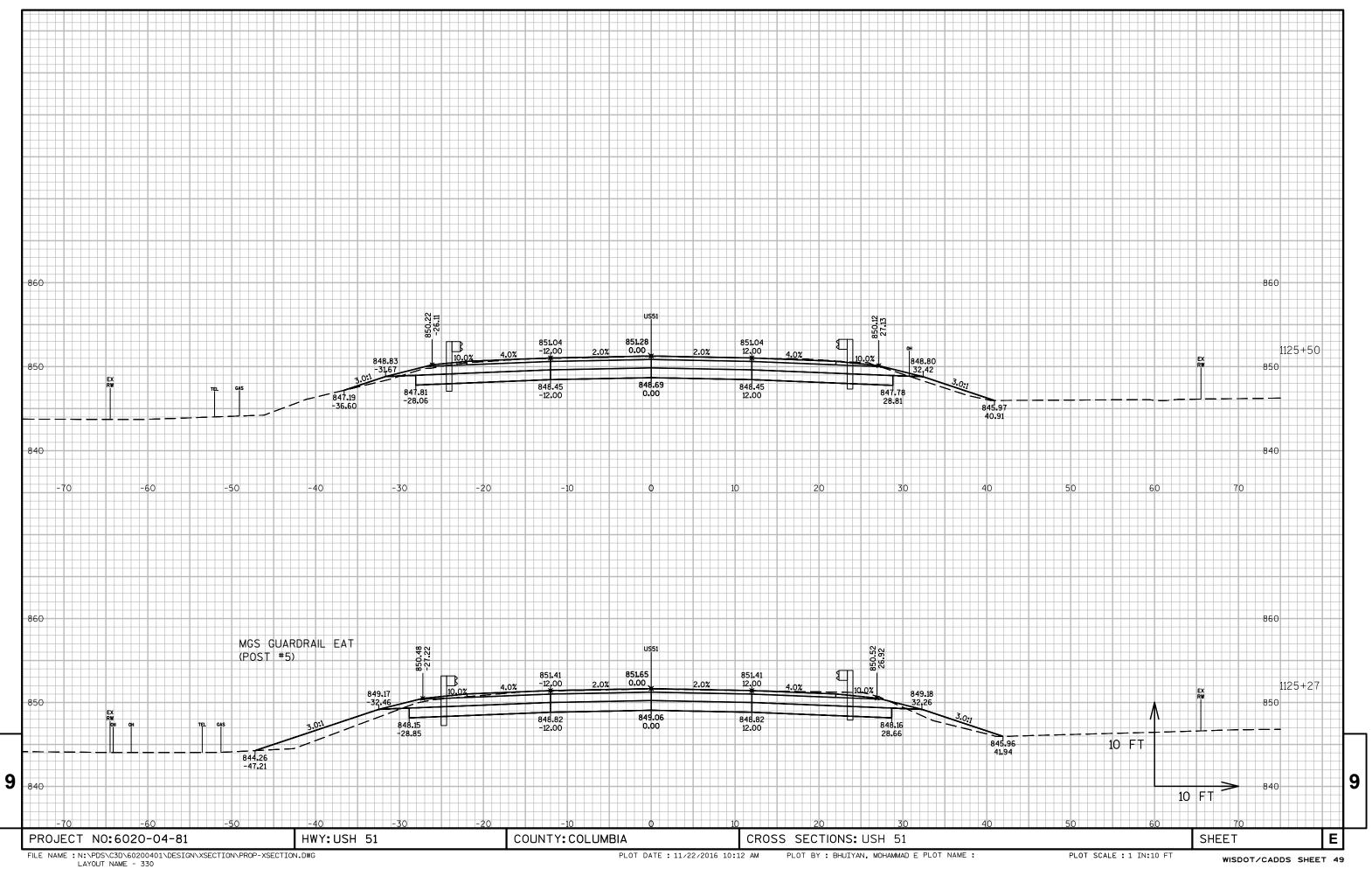
PROJECT NO:6020-04-81 HWY:USH 51 COUNTY:COLUMBIA EARTHWORK - USH 51 SHEET **E**

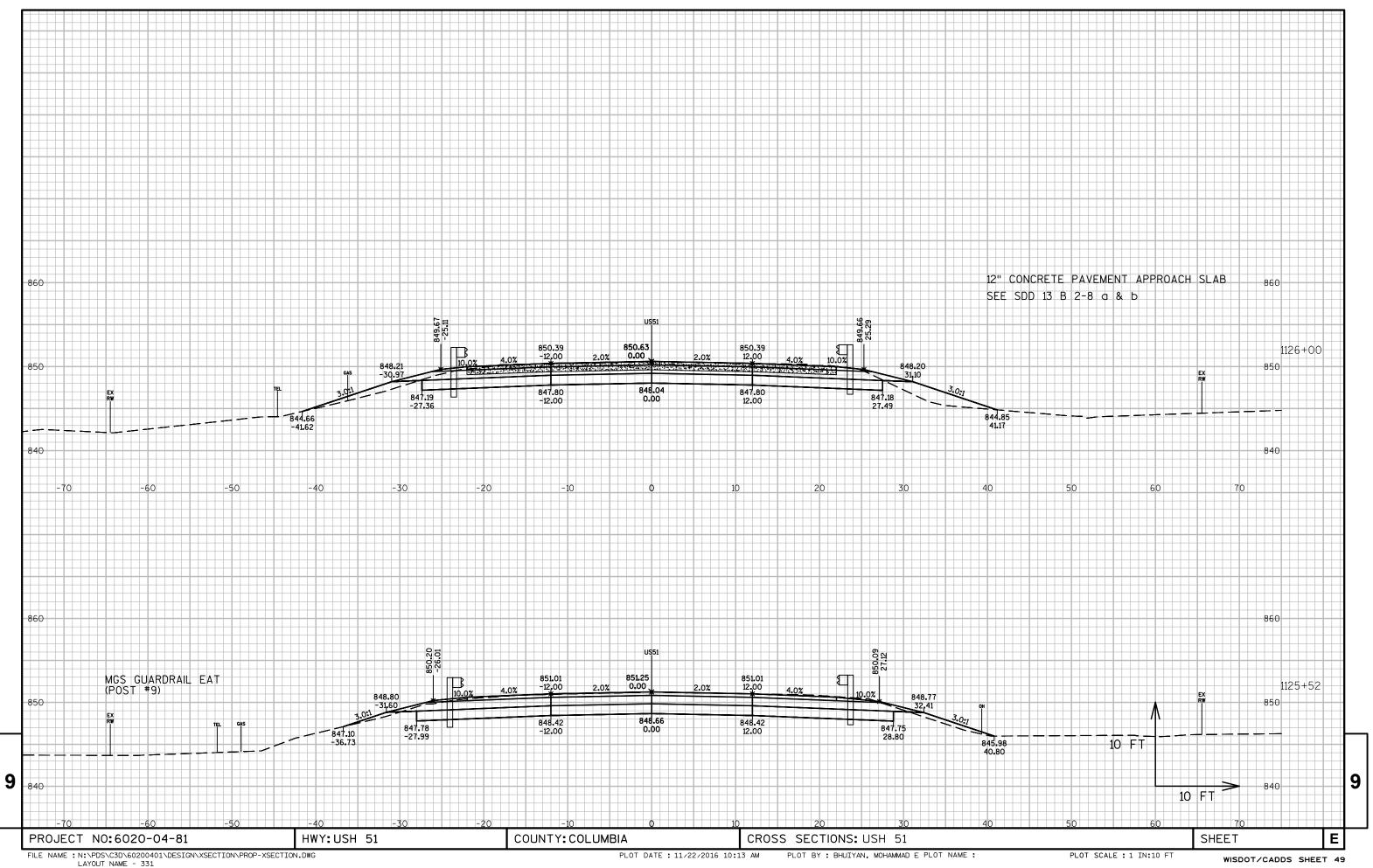


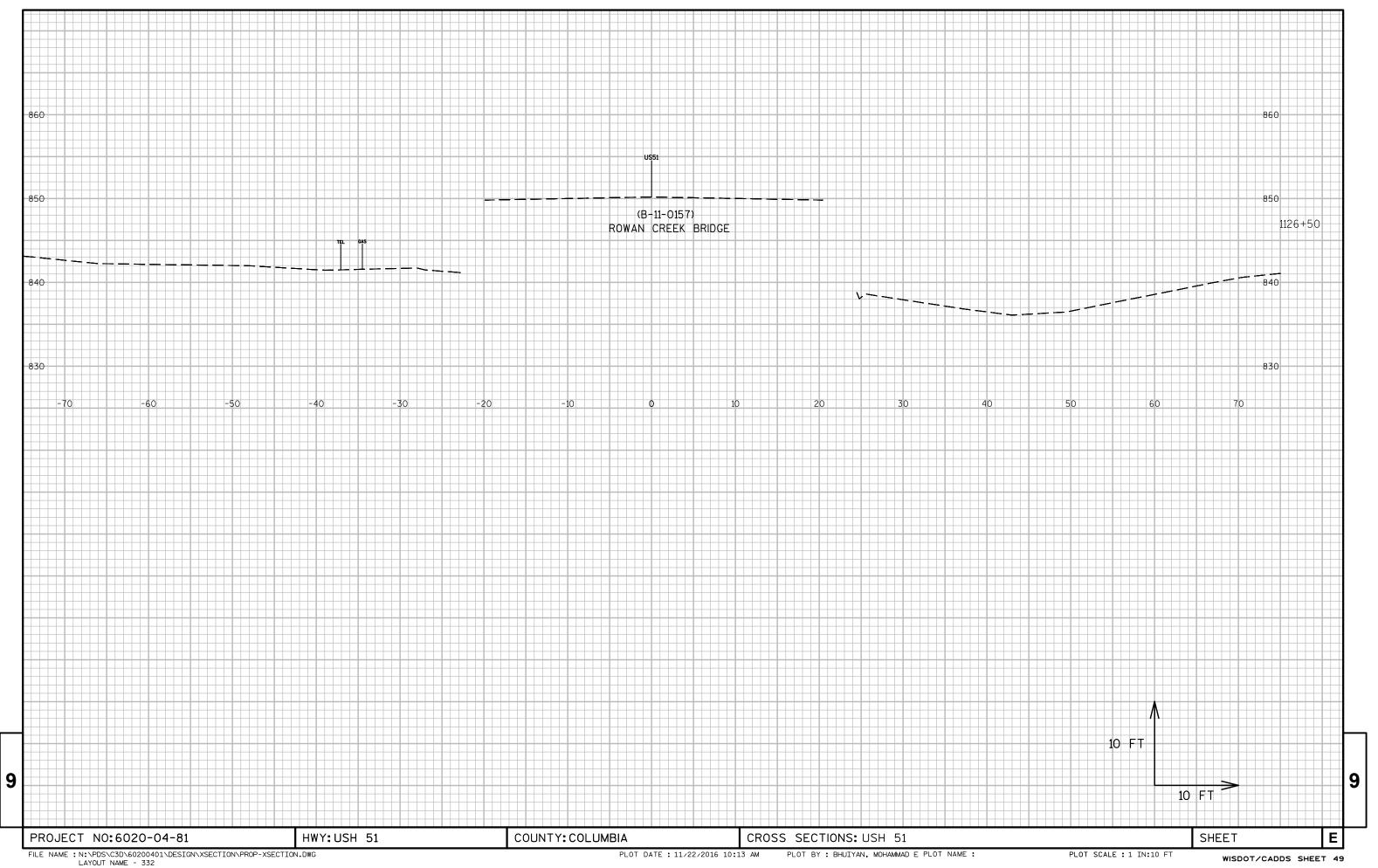


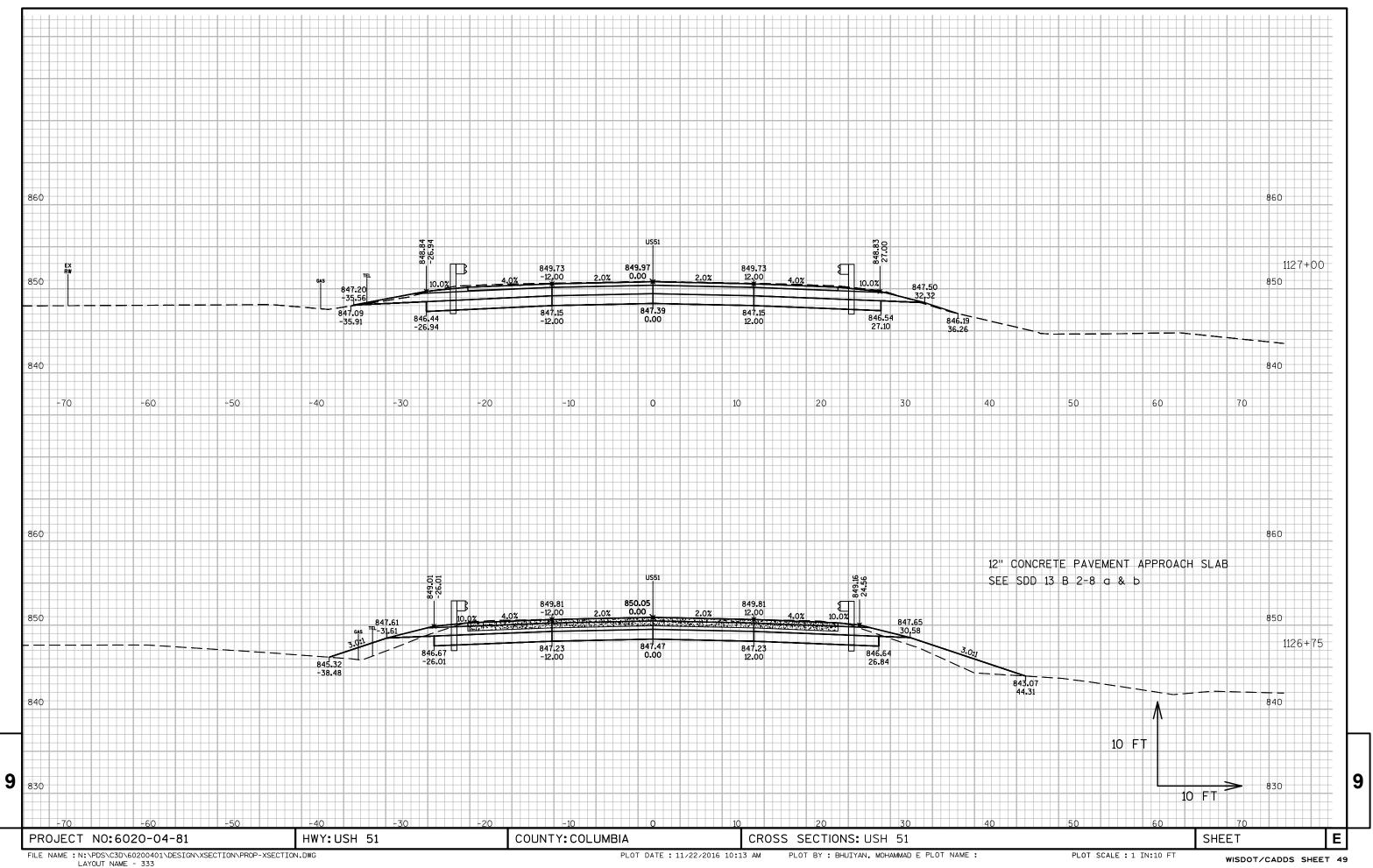


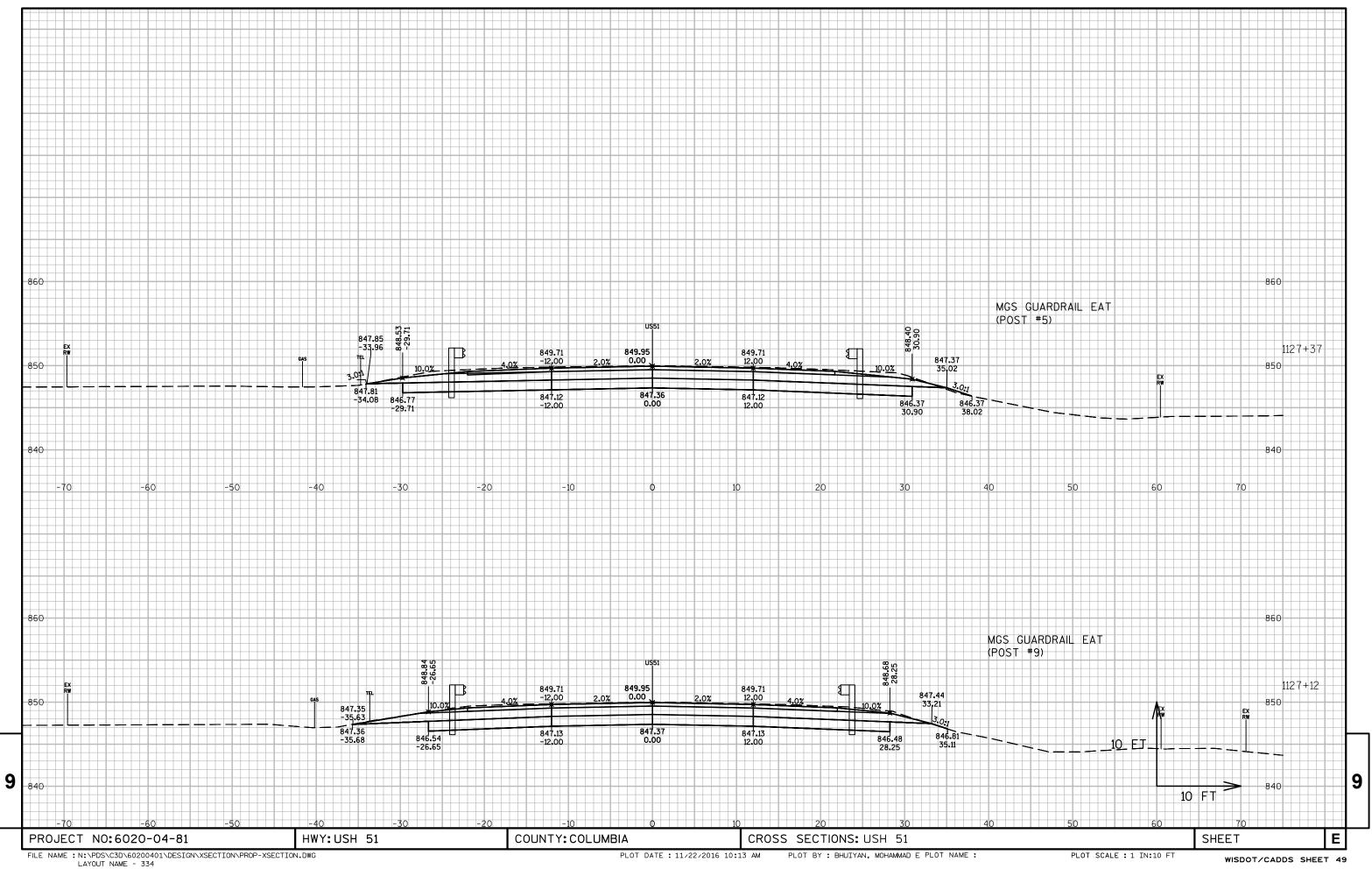


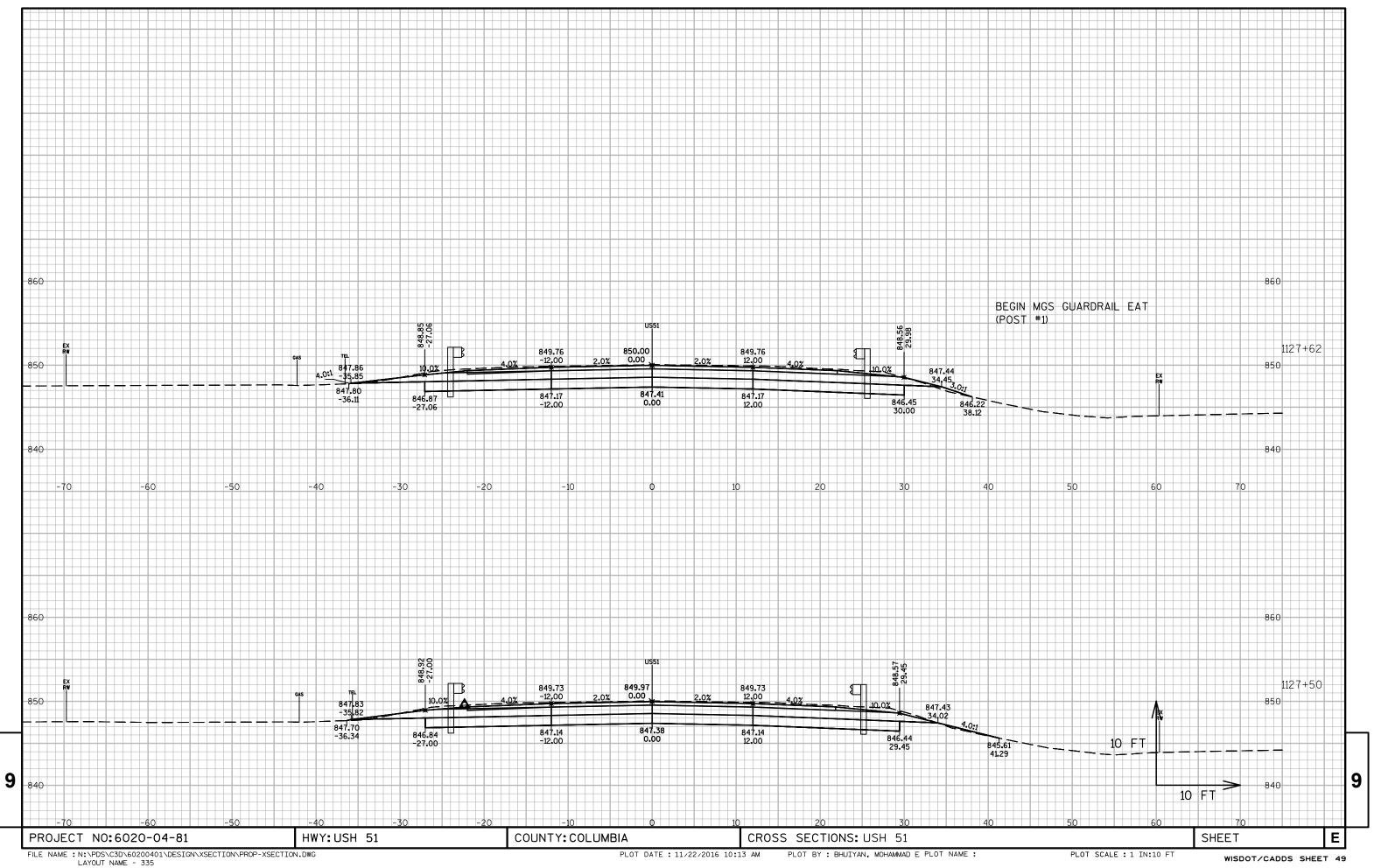


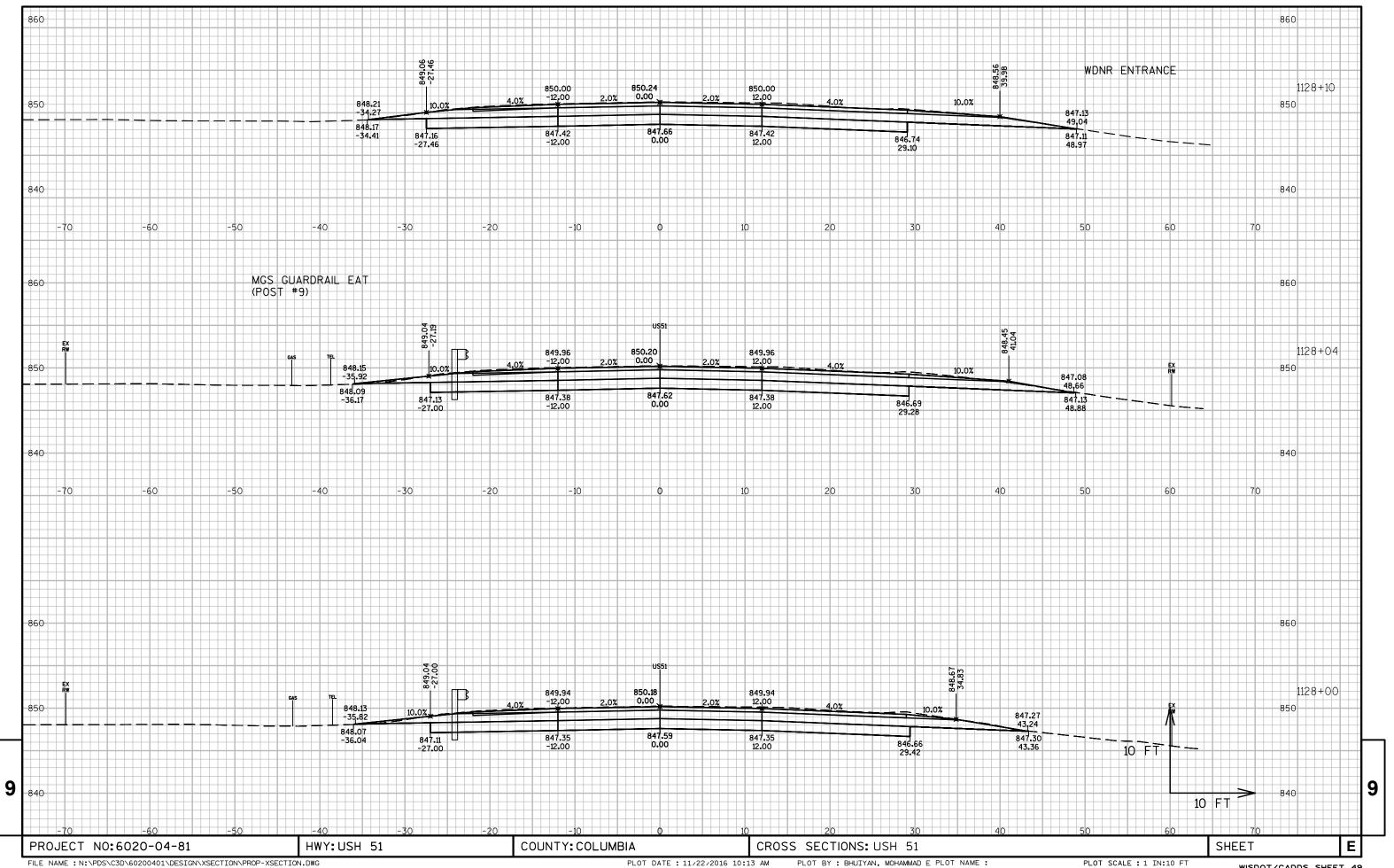


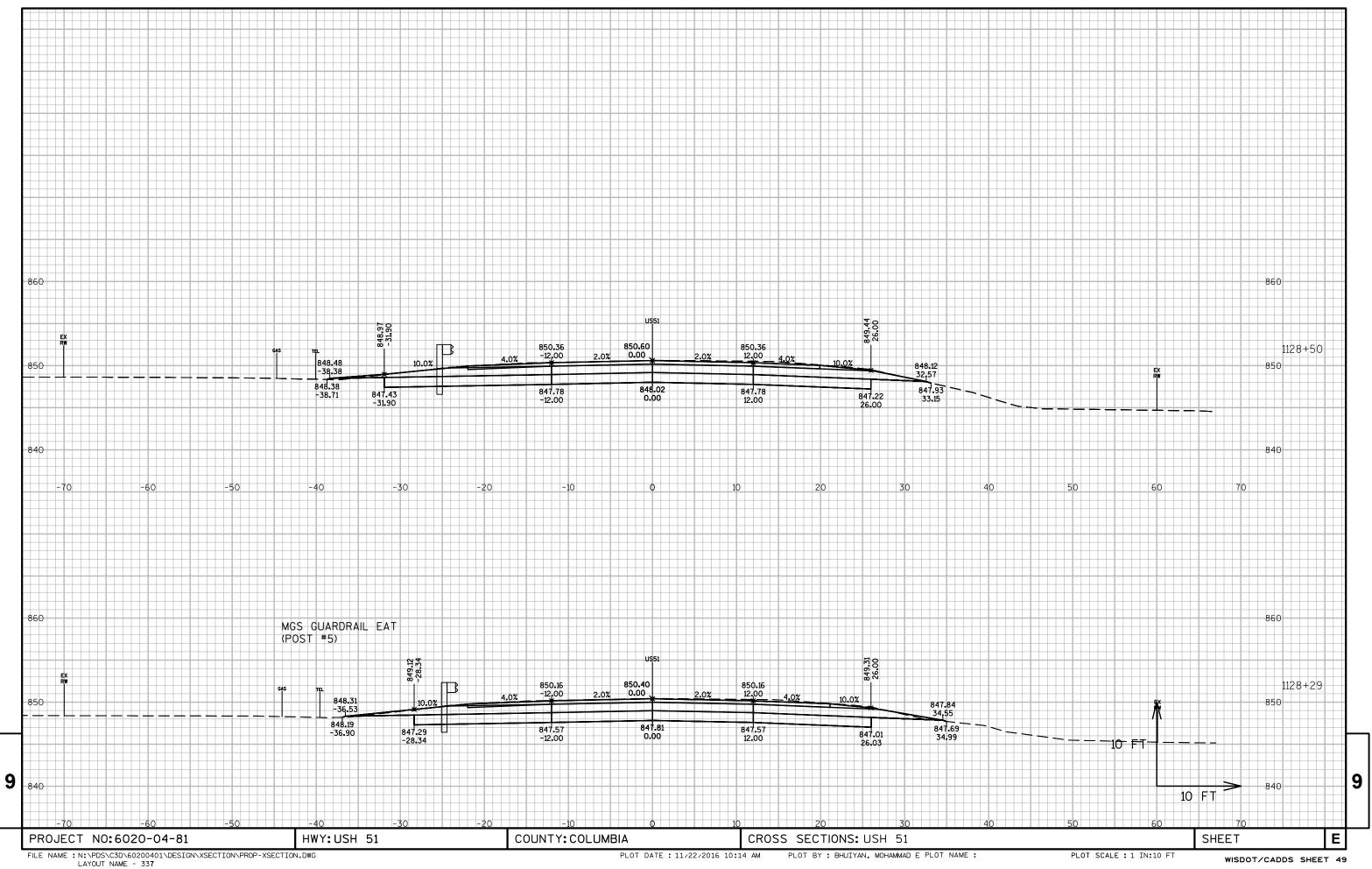


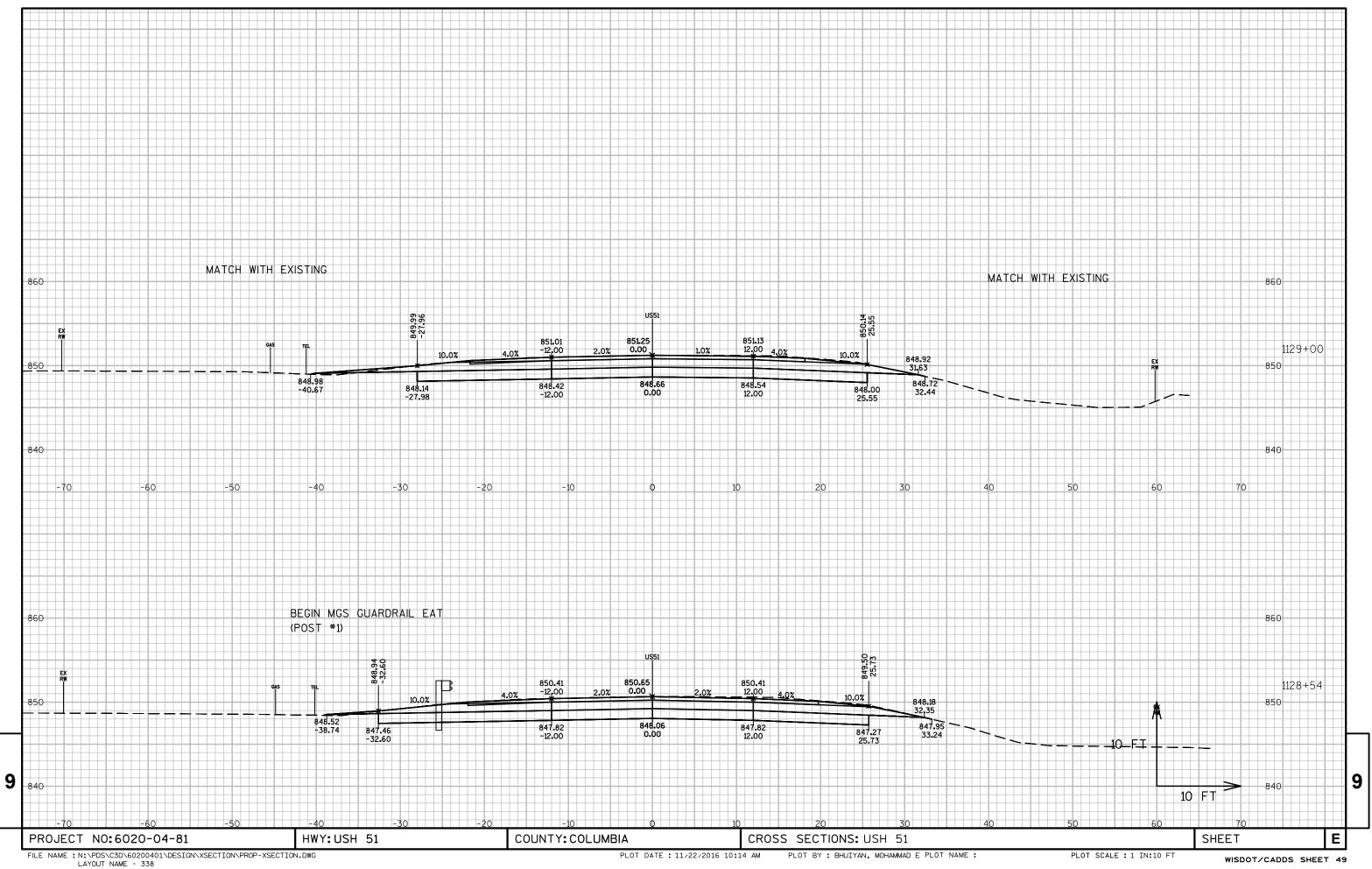














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