SUP

PROJECT ID: 8160-03-70

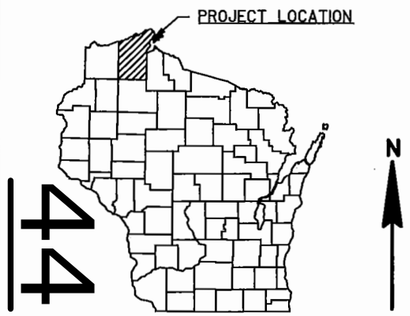
WITH: N/A

COUNTY: BAYFIELD

MAY 2017

ORDER OF SHEETS

TOTAL SHEETS = 266



DESIGN DESIGNATION 8160-03-01

A.A.D.T.	(2017)	= 5,200		
A.A.D.T.	(2027)	= 6,200		
D.H.V.		= 2,130		
D.D.		= 61/39		
T.		= 10.0%	(STA. 560+00 - 583+00'N')	(STA. 583+00 - E.O.P)
DESIGN SPEED		= 55 MPH	45 MPH	25 MPH
ESALS		= 3,200,000		

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	ROCK
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	LABEL
EXISTING RIGHT OF WAY		GRADE ELEVATION	95.36
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE		ELECTRIC	E
EXISTING CULVERT		FIBER OPTIC	FO
PROPOSED CULVERT (Box or Pipe)		GAS	G
COMBUSTIBLE FLUIDS	CAUTION	SANITARY SEWER	SAN
		STORM SEWER	SS
MARSH AREA		TELEPHONE	T
		WATER	W
		UTILITY PEDESTAL	
		POWER POLE	
WOODED OR SHRUB AREA		TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

ASHLAND - BAYFIELD

SUPERIOR AVENUE TO 7TH STREET

STH 13

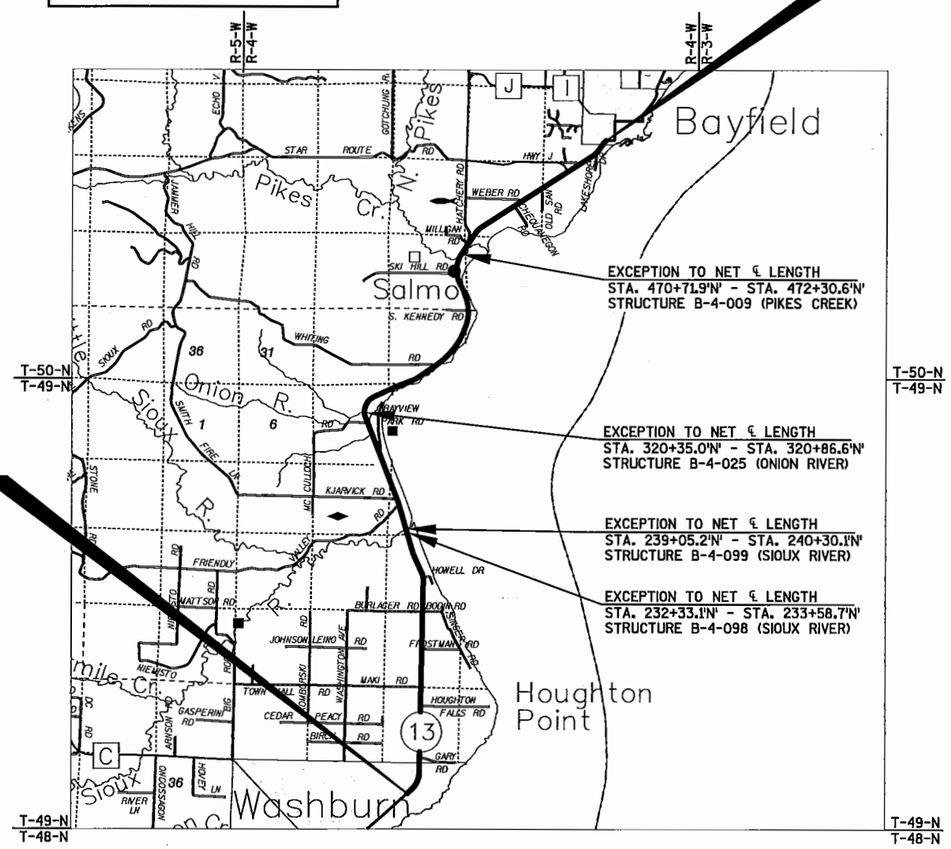
BAYFIELD

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8160-03-70	WISC 2017296	1

STATE PROJECT NUMBER
8160-03-70

BEGIN PROJECT
STA. 37+00.00'N'
X = 818,683.34
Y = 491,488.77

END PROJECT
STA. 602+84.21'N'



LAYOUT

SCALE 0 2 MILE

TOTAL NET LENGTH OF CENTERLINE = 10.717 MILES

EXCEPTION TO NET ℄ LENGTH
STA. 470+71.9'N' - STA. 472+30.6'N'
STRUCTURE B-4-009 (PIKES CREEK)

EXCEPTION TO NET ℄ LENGTH
STA. 320+35.0'N' - STA. 320+86.6'N'
STRUCTURE B-4-025 (ONION RIVER)

EXCEPTION TO NET ℄ LENGTH
STA. 239+05.2'N' - STA. 240+30.1'N'
STRUCTURE B-4-099 (SIOUX RIVER)

EXCEPTION TO NET ℄ LENGTH
STA. 232+33.1'N' - STA. 233+58.7'N'
STRUCTURE B-4-098 (SIOUX RIVER)

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, BAYFIELD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ORIGINAL PLAN PREPARED BY

N.W.B.E.
NORTHERN WISCONSIN-BASED ENGINEERS, INC.

WISCONSIN
HEATHER J. HARRINGTON
E-27174
HAYWARD
WIS.
PROFESSIONAL ENGINEER

DATE: 1/16/17

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	NWBE, INC.
Designer	NWBE, INC.
Project Manager	PHILIP KEPPERS
Regional Examiner	TOU YANG
Regional Supervisor	DAVID OSTROWSKI

APPROVED FOR THE DEPARTMENT

DATE: 1/17/2017 Philip L Keppers
(Signature)

E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\010101_TI.DWG

PLOT DATE : 1/11/2017 3:53 PM

2

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN SHALL BE IN ACCORDANCE WITH THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS.

CURVE DATA IS BASED ON THE ARC DEFINITION. BEARINGS SHOWN ON THE PLAN ARE GRID BEARINGS AND ARE TO THE NEAREST SECOND.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE OR ASPHALTIC SURFACE COURSE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYERS SHOWN ON THE PLAN IS APPROXIMATE. THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGER'S HOTLINE AND/OR A DIRECT CALL TO UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGER'S HOTLINE.

RADIUS DIMENSIONS AS SHOWN ON THE PLAN ARE TO THE EDGE OF PAVEMENT. ELEVATIONS SHOWN ON INTERSECTION DETAIL SHEETS ARE AT THE EDGE OF PAVEMENT.

RESTORE SIDE ROAD INTERSECTIONS, DRIVEWAYS, AND OTHER ENTRANCES TO EXISTING SURFACE CONDITIONS UNLESS SHOWN OTHERWISE ON THE PLAN. EXACT LOCATION AND THE LIMITS OF PRIVATE ENTRANCES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

LIMITS OF ROADSIDE CLEARING ARE GENERALLY OUT TO 45 FEET RIGHT AND/OR LEFT OF CENTERLINE, EXCEPT WHERE RIGHT-OF-WAY LIMITS ARE LESS THAN 50 FEET. AT LOCATIONS WHERE RIGHT-OF-WAY IS LESS THAN 50 FEET, CLEAR TO WITHIN A MINIMUM OF 5 FEET FROM EXISTING RIGHT-OF-WAY. ONLY CUT TREES THAT HAVE TRUNKS FULLY WITHIN THE ROADSIDE CLEARING LIMITS. PRESERVE TREES WITH TRUNKS PARTIALLY WITHIN THE CLEARING LIMITS. REFER TO QUANTITY TABLE IN PLAN FOR ACTUAL LOCATIONS AND CLEARING LIMITS.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS INDICATED FOR REMOVAL BY THE ENGINEER.

SILT FENCE SHALL BE PLACED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER, AND IT SHALL BE INSTALLED PRIOR TO CONSTRUCTION.

THE EROSION CONTROL DEVICES AS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR'S ECIP AND APPROVED BY THE ENGINEER IN CONSULTATION WITH THE DNR.

DISTURBED AREAS WITHIN THE RIGHT OF WAY SHALL BE RESTORED AS DIRECTED BY THE ENGINEER.

STATIONING, DISTANCES AND OFFSETS FOR PROPOSED SIGNS SHOWN ON THE PLAN ARE APPROXIMATE. ACTUAL LOCATIONS OF SIGNS ARE TO BE COORDINATED IN THE FIELD WITH THE ENGINEER.

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

WISDOT CONTACT
WISCONSIN DEPARTMENT OF TRANSPORTATION
ATTN: PHILIP KEPPERS, PE - PROJECT MANAGER
1701 NORTH 4TH STREET
SUPERIOR, WI 54880
PHONE (715) 395-3027
EMAIL: Philip.Keppers@dot.wi.gov

WDNR CONTACT
WISCONSIN DEPARTMENT OF NATURAL RESOURCES
ATTN: SHAWN HASELEU
ENVIRONMENTAL ANALYSIS AND REVIEW SPECIALIST
810 WEST MAPLE STREET
SPOONER, WI 54801
PHONE (715) 635-4228
EMAIL: Shawn.Haseleu@wisconsin.gov

DESIGN CONTACT
NORTHERN WISCONSIN-BASED ENGINEERS (NWBE)
ATTN: HEATHER HARRINGTON, PE
P.O. BOX 328
HAYWARD, WI 54843
PHONE (715) 634-4334
EMAIL: heather@cheqnet.net

BAYFIELD COUNTY CONTACT
BAYFIELD COUNTY HIGHWAY DEPARTMENT
ATTN: THOMAS TOEPFER - COMMISSIONER
P.O. BOX 428
311 SOUTH 1ST AVENUE EAST
WASHBURN, WI 54891
PHONE (715) 373-6115
EMAIL: ttoepfer@bayfieldcounty.org

LIST OF STANDARD ABBREVIATIONS

ABUT.	ABUTMENT
AGG.	AGGREGATE
AH.	AHEAD
AADT	ANNUAL AVERAGE DAILY TRAFFIC
APPROX.	APPROXIMATE
A.E.W.	APRON END WALL
ASPH.	ASPHALTIC
BK.	BACK
BEG.	BEGIN
B.M.	BENCH MARK
C/L OR ℄	CENTER LINE
C.E.	COMMERCIAL ENTRANCE
CONC.	CONCRETE
CONSTR.	CONSTRUCTION
CO.	COUNTY
C.T.H.	COUNTY TRUNK HIGHWAY
X-SEC.	CROSS SECTION
CR.	CRUSHED
CULV.	CULVERT
C.P.	CULVERT PIPE
D.O.T.	DEPARTMENT OF TRANSPORTATION
D.H.V.	DESIGN HOUR VOLUME
DIA.	DIAMETER
DISCH. OR DIS.	DISCHARGE
E.	EAST
X	EAST GRID COORDINATE
EB	EASTBOUND
EA.	EACH
ELEC.	ELECTRIC
EL. OR ELEV.	ELEVATION
ESALS	EQUIVALENT SINGLE AXLE LOADS
E.B.S.	EXCAVATION BELOW SUBGRADE
EXIST.	EXISTING
FERT.	FERTILIZE
F.E.	FIELD ENTRANCE
FIN.	FINISHED
F.L. OR ℄	FLOW LINE
HORIZ.	HORIZONTAL
INL.	INLET
INT.	INTERSECTION
INV.	INVERT
LT.	LEFT
L.H.F.	LEFT-HAND FORWARD
L.F.	LINEAR FOOT
L.S.	LUMP SUM
MAX.	MAXIMUM
MISC.	MISCELLANEOUS
N.	NORTH
Y	NORTH GRID COORDINATE
N.E.	NORTHEAST
N.W.	NORTHWEST
PAVT.	PAVEMENT
P.C.	POINT OF CURVATURE
P.I.	POINT OF INTERSECTION
P.T.	POINT OF TANGENCY
P.O.T.	POINT ON TANGENT
P.E.	PRIVATE ENTRANCE
PROJ.	PROJECT
R.	RANGE
REQD.	REQUIRED
R/L	REFERENCE LINE
RT.	RIGHT
R.H.F.	RIGHT-HAND FORWARD
R/W	RIGHT-OF-WAY
RD.	ROAD
SHLD.	SHOULDER
S.	SOUTH
S.D.D.	STANDARD DETAIL DRAWINGS
S.T.H.	STATE TRUNK HIGHWAYS
STA.	STATION
STRUCT.	STRUCTURE
TEL.	TELEPHONE
TEMP.	TEMPORARY
TN.	TOWN
T.	TRUCKS (PERCENT OF)
TYP.	TYPICAL
U.G.	UNDERGROUND
VAR.	VARIABLE
V.	VELOCITY OR DESIGN SPEED
V.C.	VERTICAL CURVE
W.	WEST
WB	WESTBOUND
W.A.	WORKING DAY
WZ	WORK ZONE

UTILITIES

BAYFIELD ELECTRIC COOPERATIVE
ATTN: GARY TARASEWICZ
P.O. BOX 68
IRON RIVER, WI 54847
BEC OFFICE (715) 372-4287
DIRECT LINE (715) 372-7539
EMAIL: gary.tarasewicz@bayfieldelectric.com

CITY OF BAYFIELD PUBLIC WORKS
ATTN: THOMAS KOVACHEVICH - PUBLIC WORKS DIRECTOR
P.O. BOX 1170
BAYFIELD, WI 54814
PHONE (715) 779-5712
MOBILE (715) 209-3956
EMAIL: citypublicworks@cityofbayfield.com

CENTURYLINK
ALAN NICKELL
P.O. BOX 181
SOLON SPRINGS, WI 54873
PHONE (715) 378-2131
MOBILE (715) 566-3879
EMAIL: alan.nickell@centurylink.com

CHARTER COMMUNICATIONS
TOM HAASE
2304 S. MAIN ST.
RICE LAKE, WI 54868
PHONE (715) 719-0564
MOBILE (715) 418-9317
EMAIL: tom.haase@charter.com

XCEL ENERGY - ELECTRIC
ATTN: MURRAY SMERER - SENIOR DESIGNER
2400 FARM ROAD
ASHLAND, WI 54806
PHONE (715) 682-6928
EMAIL: murray.j.smerer@xcelenergy.com

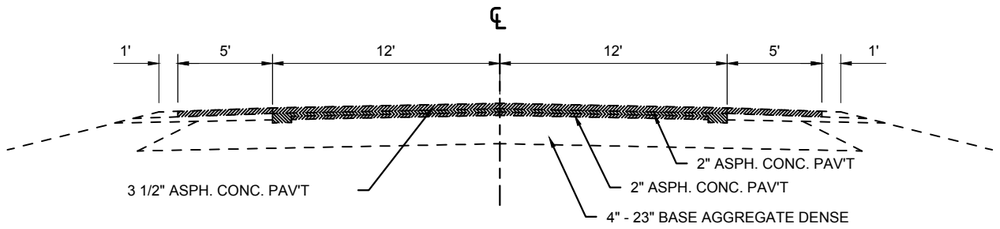
XCEL ENERGY - GAS
ATTN: MURRAY SMERER - SENIOR DESIGNER
2400 FARM ROAD
ASHLAND, WI 54806
PHONE (715) 682-6928

NORTHERN NATURAL GAS COMPANY
ATTN: JEFF TELKER
1995 N. NATURAL GAS RD.
CARLTON, MN 55718
PHONE (402) 530-3466
MOBILE (218) 348-3470
EMAIL: Jeff.Telker@NNGCO.com

NORVADO
ATTN: GUY FOLSOM
P.O. BOX 67
CABLE, WI 54821
PHONE (715) 798-7123
MOBILE (715) 580-8123
EMAIL: gfolsom@norvado.com

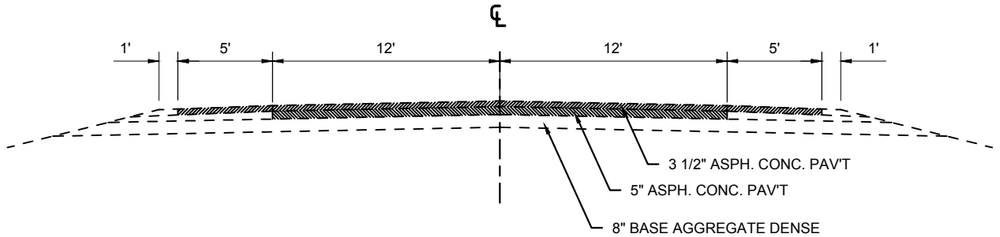
PIKES BAY SANITARY DISTRICT
ATTN: REX DOLLINGER - PRESIDENT
P.O. BOX 689
BAYFIELD, WI 54814
PHONE (715) 779-9866
EMAIL: rex.dollinger@yahoo.com

WASHBURN MUNICIPAL WATER AND SEWER UTILITY
ATTN: GAYLA SALMI - DIRECTOR OF PUBLIC WORKS
P.O. BOX 638
WASHBURN, WI 54891
PHONE (715) 373-6171
AFTER HOURS DISPATCH (715) 373-6120
EMAIL: dpw@cityofwashburn.org



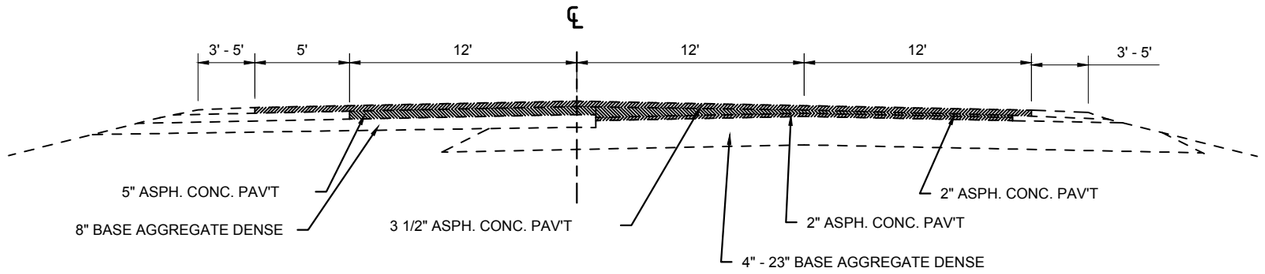
TYPICAL EXISTING SECTION

STA. 37+00'N' - 70+00'N'
STA. 77+59'N' - 232+33'N'
STA. 233+59'N' - 239+05'N'
STA. 240+22'N' - 314+50'N'
STA. 422+00'N' - 439+00'N'
STA. 440+50'N' - 442+00'N'
STA. 453+00'N' - 454+16'N'
STA. 459+44'N' - 470+72'N'
STA. 472+31'N' - 486+90'N'
STA. 550+00'N' - 572+00'N'
STA. 585+00'N' - 601+70'N'



TYPICAL EXISTING SECTION

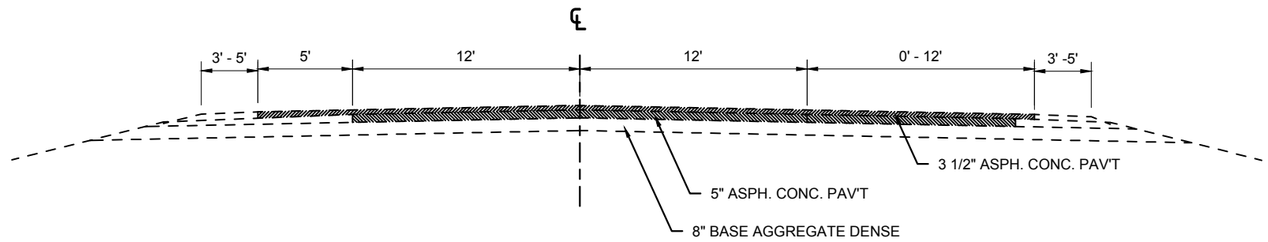
STA. 70+00'N' - 77+59'N'
STA. 314+50'N' - 320+35'N'
STA. 320+87'N' - 330+85'N'
STA. 391+00'N' - 398+00'N'
STA. 545+00'N' - 550+00'N'
STA. 572+00'N' - 585+00'N'



TYPICAL EXISTING SECTION

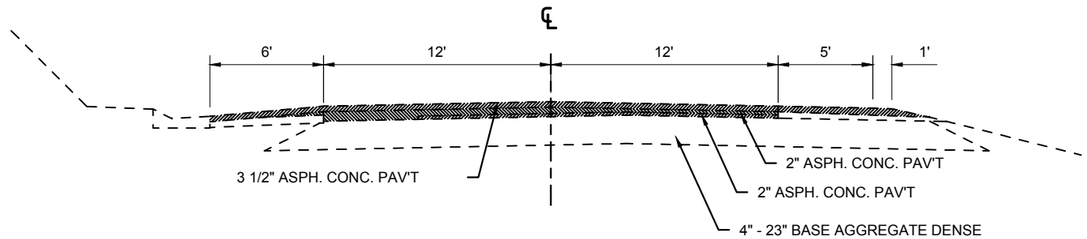
STA. 337+00'N' - 351+00'N'
STA. 486+90'N' - 539+30'N'

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: TYPICAL SECTIONS	SHEET 3	E



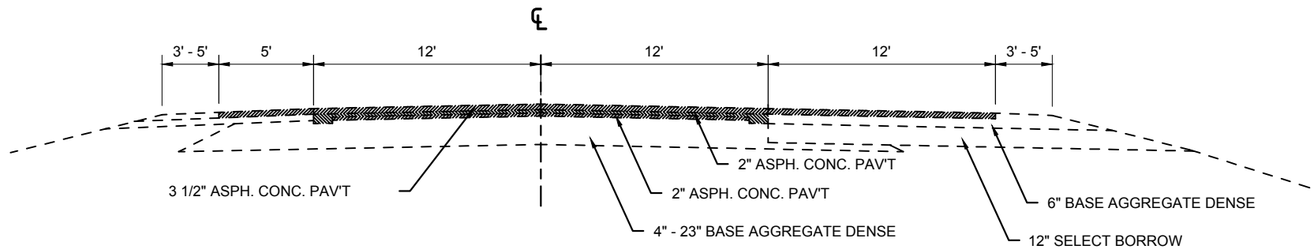
TYPICAL EXISTING SECTION

STA. 330+85'N' - 337+00'N'
STA. 351+00'N' - 357+70'N'
STA. 479+00'N' - 486+90'N'
STA. 532+50'N' - 535+01'N' (ASPHALT CONC. SHOULDER AND CURB & GUTTER)
STA. 539+30'N' - 545+00'N'



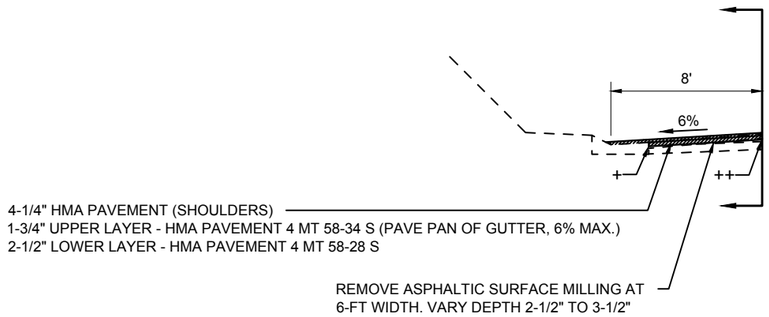
TYPICAL EXISTING SECTION

STA. 371+13'N' - 379+00'N'
STA. 379+89'N' - 382+14'N'
STA. 384+14'N' - 389+19'N'
STA. 393+35'N' - 395+48'N'
STA. 397+48'N' - 411+12'N'
STA. 414+22'N' - 415+38'N'
STA. 417+50'N' - 419+26'N'
STA. 439+68'N' - 440+56'N'
STA. 532+50'N' - 535+01'N'



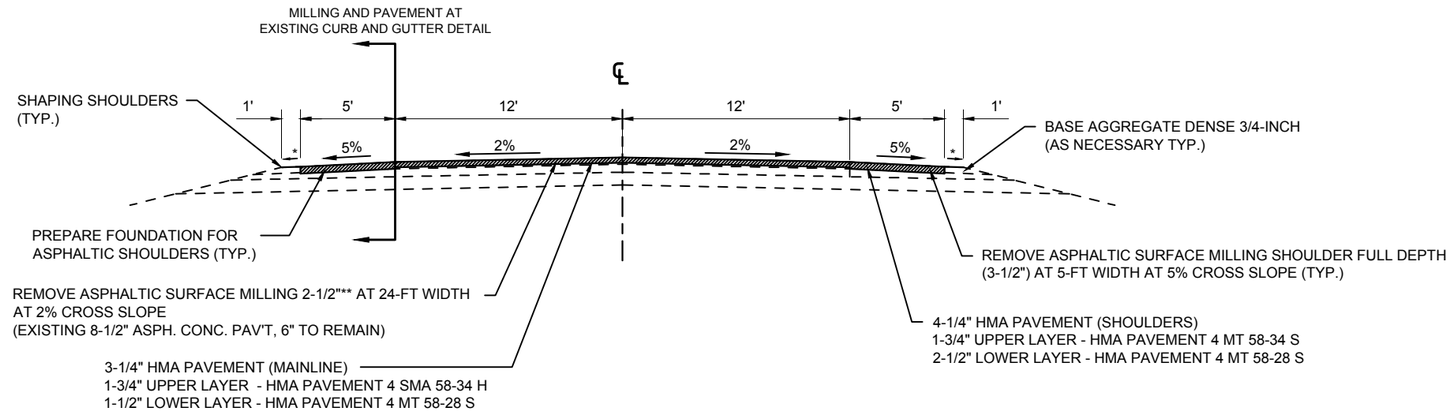
TYPICAL EXISTING SECTION

STA. 454+16'N' - 459+44'N'



MILLING AND PAVMENT AT EXISTING CURB & GUTTER SECTION DETAIL

STA. 371+13'N' - 379+00'N'
STA. 379+89'N' - 382+14'N'
STA. 384+14'N' - 389+19'N'
STA. 393+35'N' - 395+48'N'
STA. 397+48'N' - 411+12'N'
STA. 414+22'N' - 415+38'N'
STA. 417+50'N' - 419+26'N'
STA. 439+68'N' - 440+56'N'
STA. 532+50'N' - 535+01'N'



PROPOSED TYPICAL SECTION

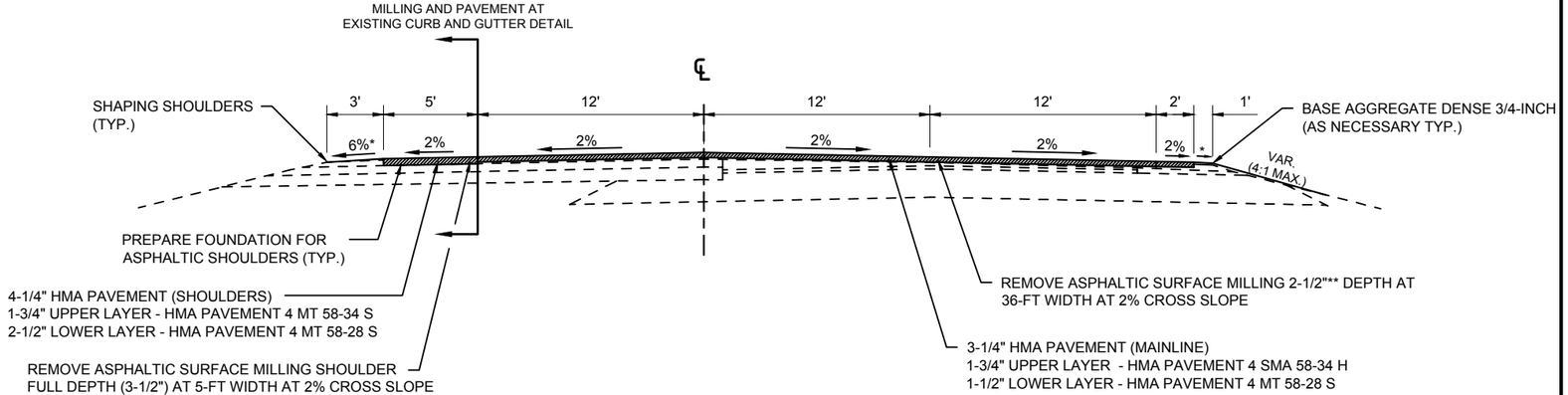
STA. 37+00'N' - 232+33'N'
STA. 233+59'N' - 239+05'N'
STA. 240+30'N' - 320+35'N'
STA. 320+87'N' - 330+85'N'
STA. 357+70'N' - 454+00'N'
STA. 459+60'N' - 470+72'N'
STA. 472+31'N' - 479+00'N'
STA. 544+00'N' - 602+84'N'

NOTE:

* VARY FINISHED BASE AGGREGATE DENSE SHOULDER SLOPE AS NEEDED TO MATCH EXISTING SHOULDER POINT (6% MAX.). SHOULDER BASE AGGREGATE MAY EXTEND ON TO ROADWAY INSLOPE (4:1 MAX.).

** MEASURE MILLING DEPTH AT EDGE OF EXISTING 12' TRAFFIC LANE OR 24' PASSING / BY-PASS LANE.

+ MILL DEPTH 2-1/2". ++ MILL DEPTH 3-1/2".

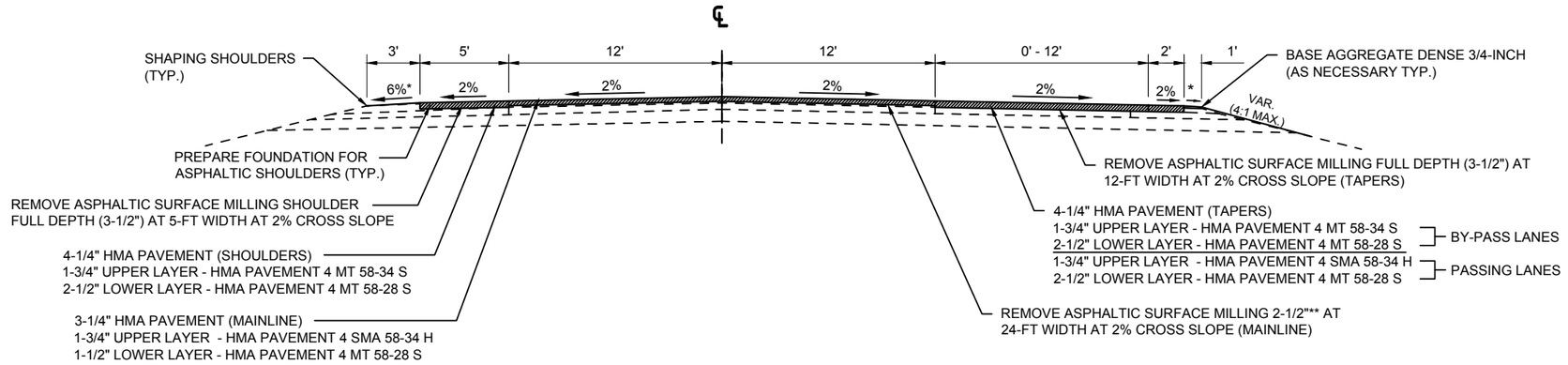


PROPOSED TYPICAL SECTION (PASSING LANES)

STA. 337+00'N' - 351+00'N'
STA. 486+90'N' - 537+80'N'

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: TYPICAL SECTIONS	SHEET 5	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\020301_TS.DWG
1229_RESURFACE_020301_TS.DST - 03
PLOT DATE : 2/12/2016 7:36 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN : 8 FT
WISDOT/CADDS SHEET 42



PROPOSED TYPICAL SECTION (TAPERS)

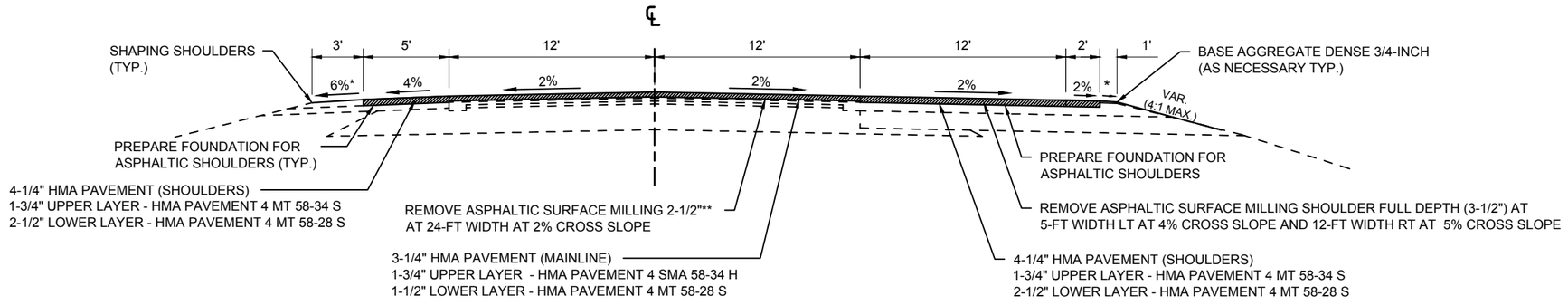
STA. 330+85'N' - 337+00'N'
STA. 351+00'N' - 371+13'N'
STA. 454+00'N' - 455+35'N'***
STA. 458+24'N' - 459+60'N'***
STA. 479+00'N' - 486+90'N'
STA. 537+80'N' - 544+00'N'

NOTE:

* VARY FINISHED BASE AGGREGATE DENSE SHOULDER SLOPE AS NEEDED TO MATCH EXISTING SHOULDER POINT (6% MAX.). SHOULDER BASE AGGREGATE MAY EXTEND ON TO ROADWAY INSLOPE (4:1 MAX.).

** MEASURE MILLING DEPTH AT EDGE OF EXISTING 12' TRAFFIC LANE OR 24' PASSING / BY-PASS LANE.

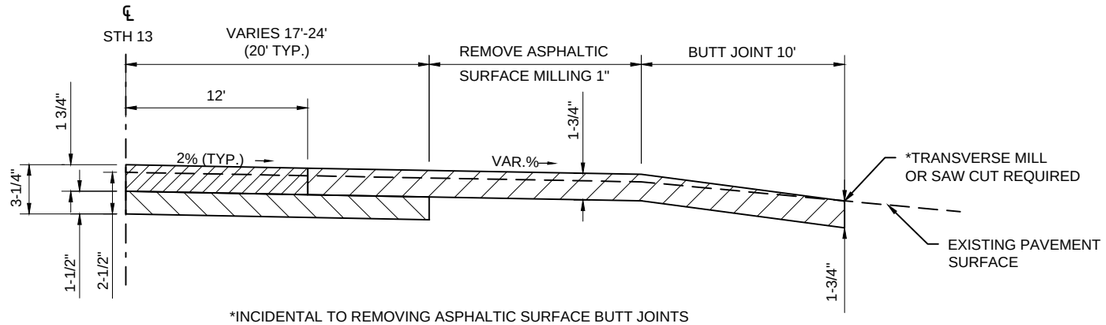
*** STATIONING OF BY-PASS LANE TAPERS.



PROPOSED TYPICAL SECTION (BY-PASS LANE)

STA. 455+35'N' - 458+24'N'

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: TYPICAL SECTIONS	SHEET 6	E



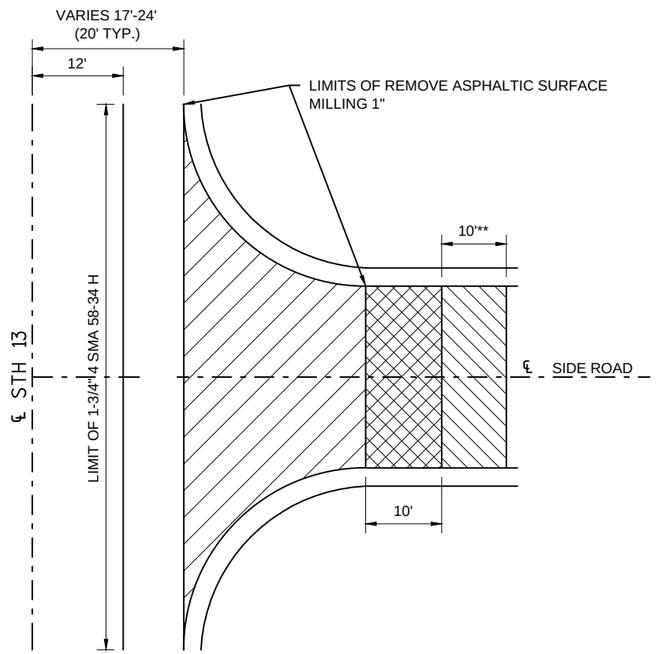
*INCIDENTAL TO REMOVING ASPHALTIC SURFACE BUTT JOINTS

NOTES:

UPPER LAYER 1-3/4" HMA PAVEMENT 4 SMA 58-34 H

UPPER LAYER 1-3/4" HMA PAVEMENT 4 MT 58-34 S

LOWER LAYER 1-1/2" HMA PAVEMENT 4 MT 58-28 S



NOTES:

REMOVING ASPHALTIC SURFACE MILLING 1", HMA PAVEMENT 4 MT 58-34 S

REMOVING ASPHALTIC SURFACE BUTT JOINT

3/4-INCH BASE AGGREGATE DENSE,**
FEATHER TO MATCH PAVED SIDE ROAD APRONS.

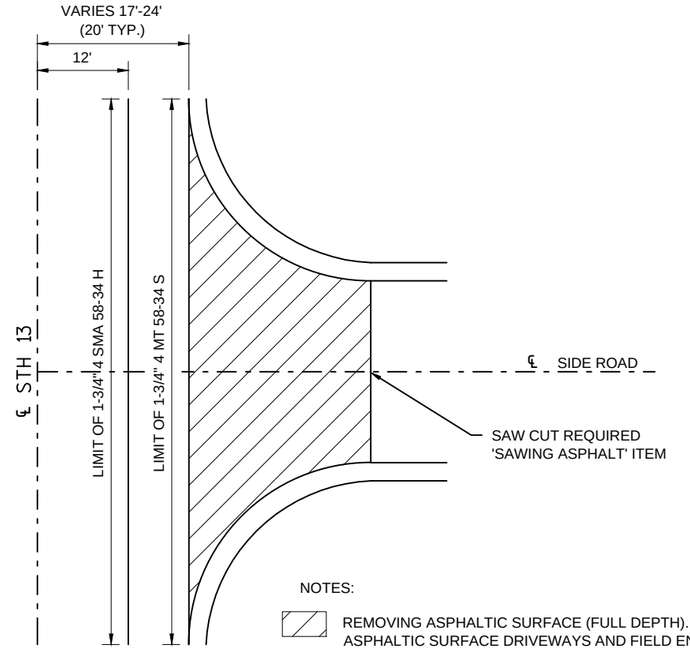
MATCH EXISTING PAVED SURFACE RADII AND TAPERS.

4" HMA PAVEMENT 4 MT 58-34 S REQUIRED ON SIDE ROADS
NOT PAVED TO THE RADIUS POINTS.

**REQUIRED ON UN-PAVED SIDE ROADS ONLY.

SOUTH KENNEDY ROAD IS AN UN-IMPROVED SIDE ROAD. EXCAVATE TO THE HORIZONTAL LIMITS AS DIRECTED BY THE ENGINEER, TO A DEPTH OF 12". PLACE AND COMPACT 8" OF BASE AGGREGATE AND 4" OF HMA PAVEMENT AS DIRECTED BY THE ENGINEER. BASE AGGREGATE PAID UNDER ITEM 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH. EXCAVATION PAID UNDER ITEM 205.9015.S GRADING SHAPING AND FINISHING INTERSECTION.

PAVED AND UN-PAVED SIDE ROAD



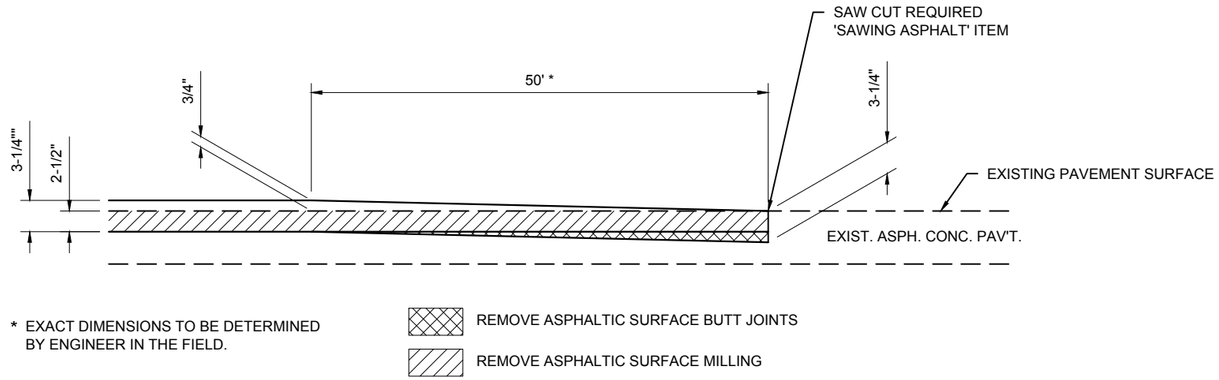
NOTES:

REMOVING ASPHALTIC SURFACE (FULL DEPTH). 2-1/2" LAYER*.
ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES.

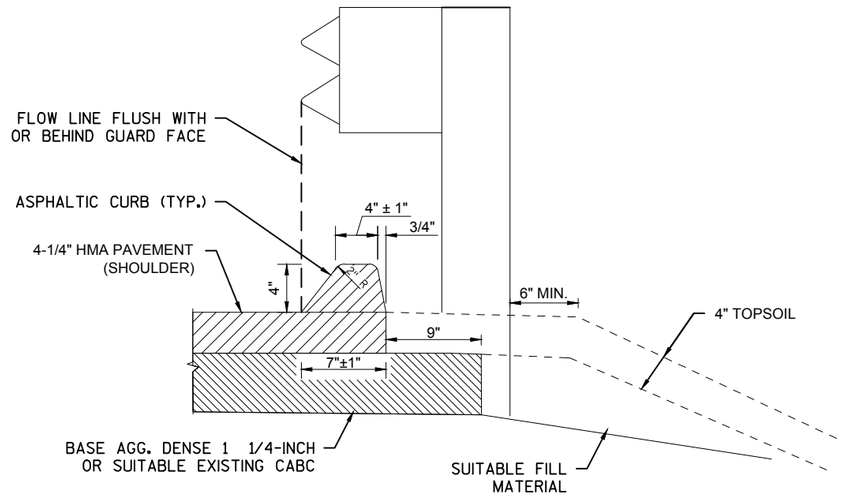
MATCH EXISTING PAVED SURFACE RADII AND TAPERS.
*MATCH EXISTING PAVEMENT THICKNESS.

PAVED DRIVEWAYS

2



MAINLINE BUTT JOINT DETAIL

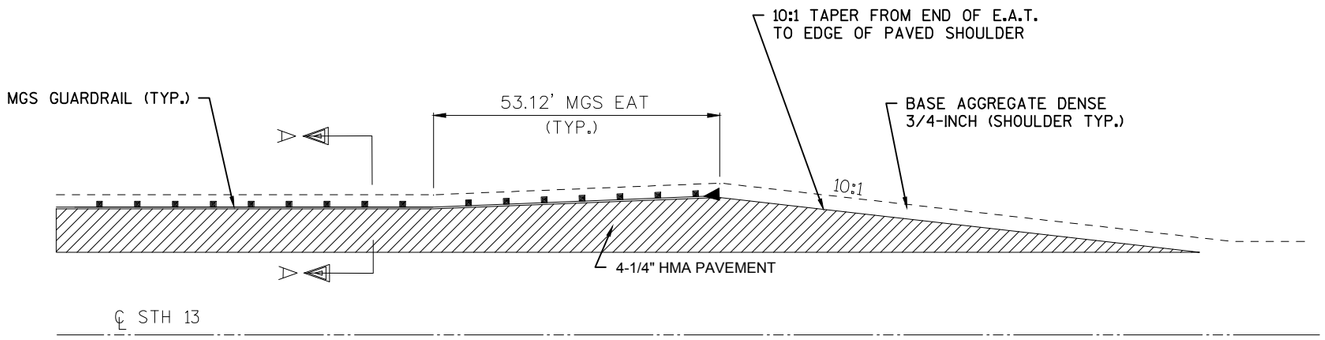


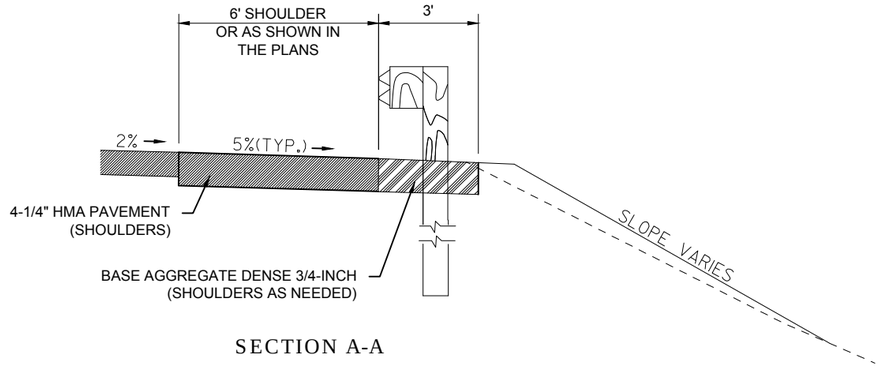
NOTE:

ASPHALTIC CURB SHALL ONLY BE CONSTRUCTED BETWEEN POST NO. 3 OF THE E.A.T.'s OF THIS BEAM GUARD INSTALLATION.

ASPHALTIC CURB DETAIL

STA 598+98.5 - 601+46.1 RT





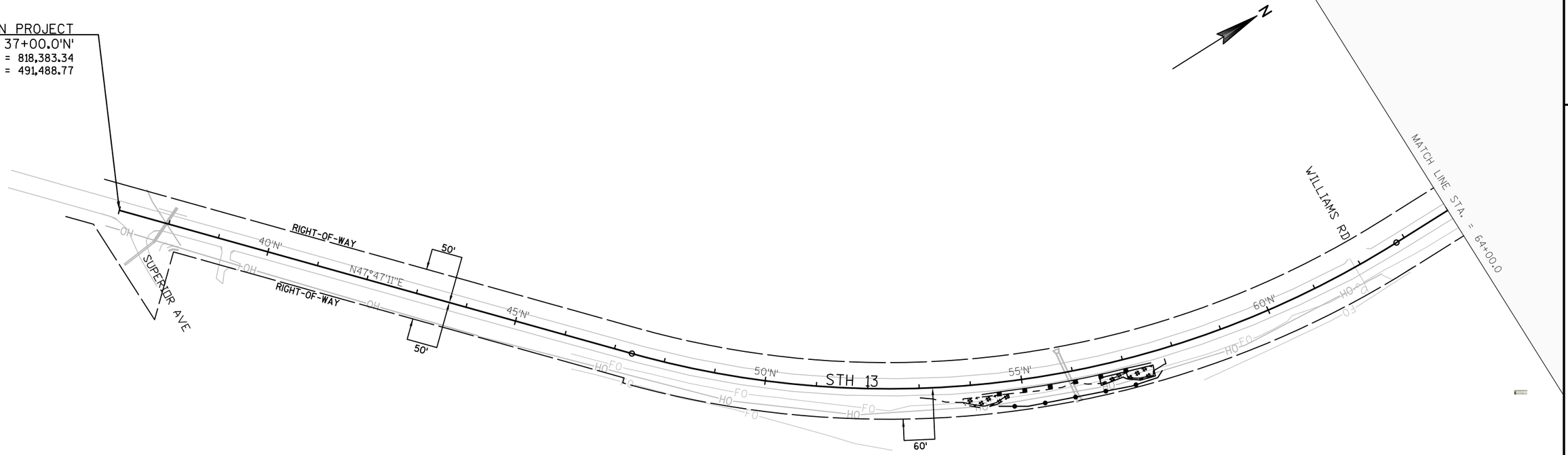
SECTION A-A

HMA PAVEMENT AT BEAM GUARD DETAIL

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: CONSTRUCTION DETAILS	SHEET 9	E

2

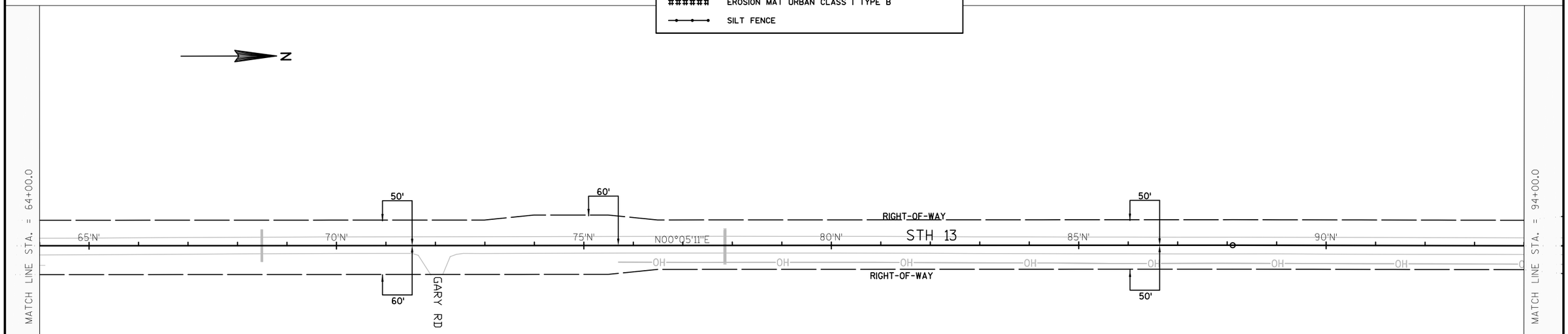
BEGIN PROJECT
STA. 37+00.0'N'
X = 818,383.34
Y = 491,488.77



LEGEND

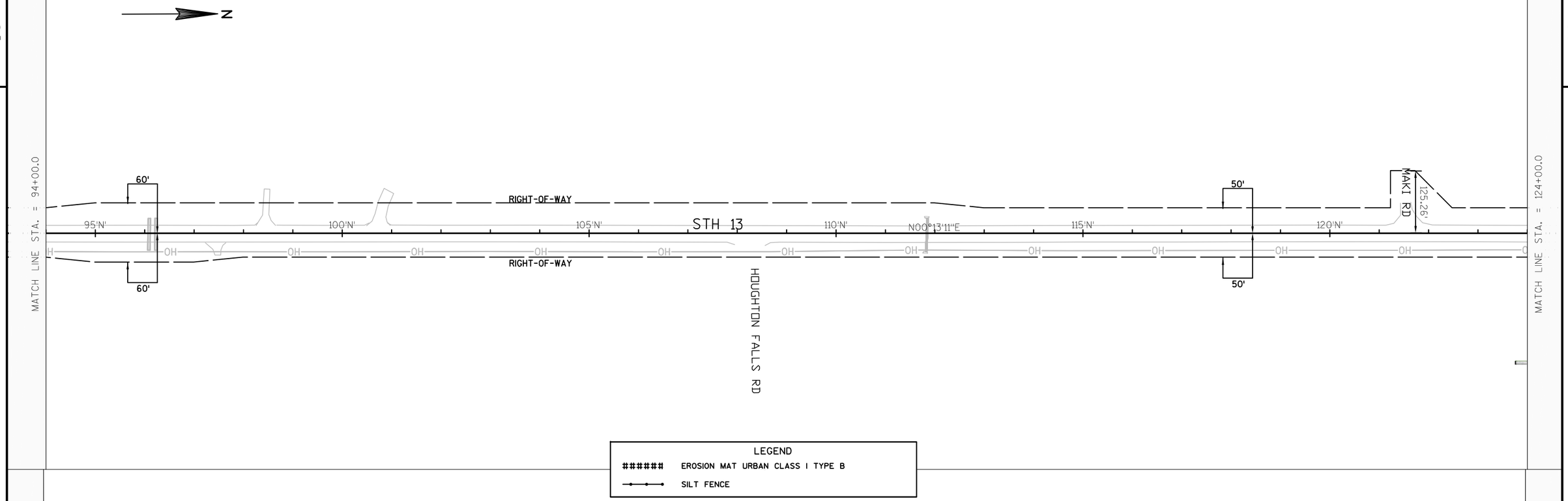
EROSION MAT URBAN CLASS I TYPE B

SILT FENCE



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 10	E

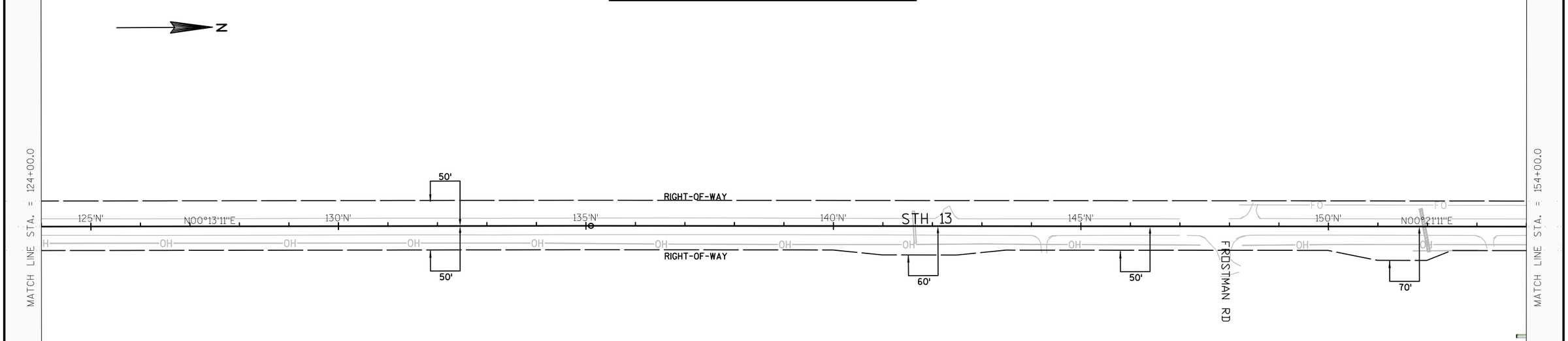
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\022001_EC_STH13.DWG
LAYOUT NAME - 022001_EC_STH13 - STH13_PN-1
PLOT DATE : 1/27/2017 10:09 AM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT



LEGEND

EROSION MAT URBAN CLASS I TYPE B

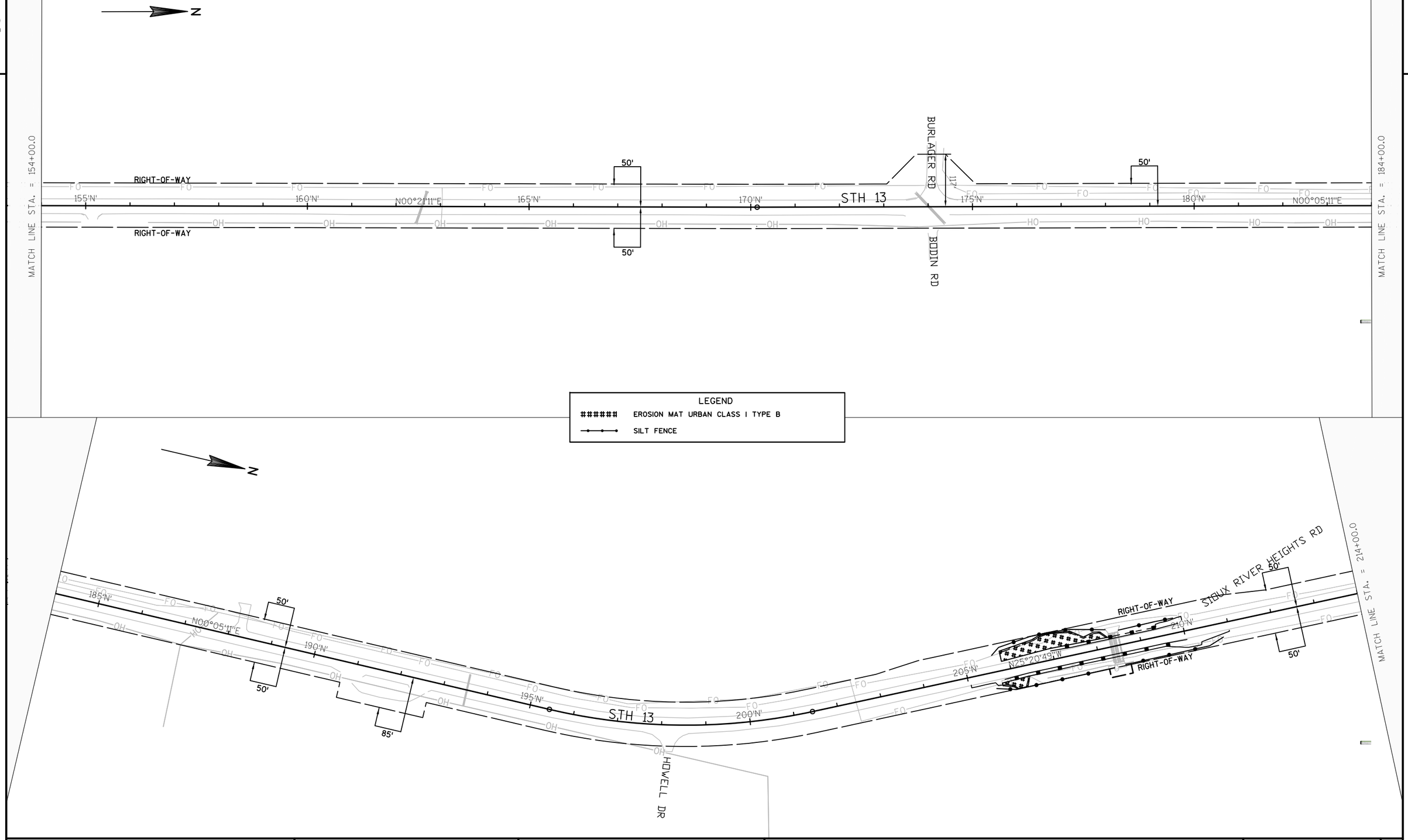
SILT FENCE



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 11	E

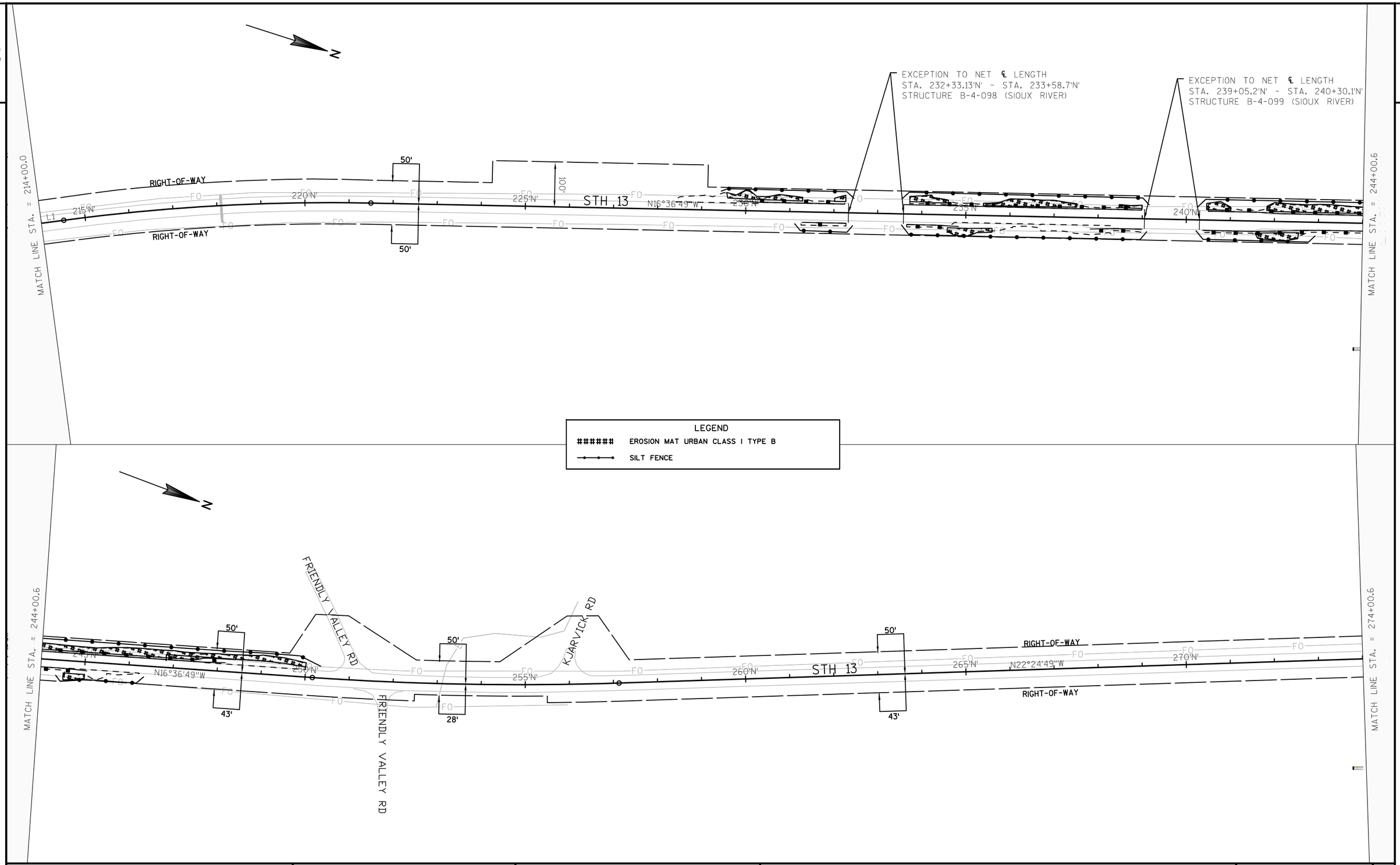
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\022001_EC_STH13.DWG
LAYOUT NAME - 022001_EC_STH13 - STH13_PN-3
PLOT DATE : 1/27/2017 10:10 AM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT
WISDOT/CADDS SHEET 44

2



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 12	E

2



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 13	E

2

2

MATCH LINE STA. = 274+00.6

MATCH LINE STA. = 304+00.6

N

RIGHT-OF-WAY

50'

43'

FO

275'N'

280'N'

285'N'

N22°24'49"W

290'N'

STH 13

295'N'

300'N'

RIGHT-OF-WAY

BAYVIEW PARK RD

LEGEND

EROSION MAT URBAN CLASS I TYPE B

SILT FENCE

N

MATCH LINE STA. = 304+00.6

305'N'

RIGHT-OF-WAY

N22°24'49"W

310'N'

50'

43'

MCCULLOCH RD

315'N'

320'N'

325'N'

STH 13

330'N'

HO

52'

EXCEPTION TO NET ℄ LENGTH
STA. 320+35.0'N' - STA. 320+86.6'N'
STRUCTURE B-4-025 (ONION RIVER)

MATCH LINE STA. = 334+76.8

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 14	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\022001_EC_STH13.DWG
LAYOUT NAME - 022001_EC_STH13 - STH13_PN-9

PLOT DATE : 1/27/2017 10:11 AM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT

2

2

N

MATCH LINE STA. = 334+76.8

88'

52'

RIGHT-OF-WAY

75'

RIGHT-OF-WAY

STH 13

335'N'

340'N'

N64°37'11"E

345'N'

350'N'

355'N'

360'N'

N69°04'11"E

65'

HO

FO

TFO

LEGEND

####### EROSION MAT URBAN CLASS I TYPE B

SILT FENCE

N

MATCH LINE STA. = 364+59.3

365'N'

370'N'

N54°25'11"E

375'N'

380'N'

385'N'

N49°49'11"E

390'N'

N45°59'11"E

65'

RIGHT-OF-WAY

RIGHT-OF-WAY

STH 13

WHITING RD

50'

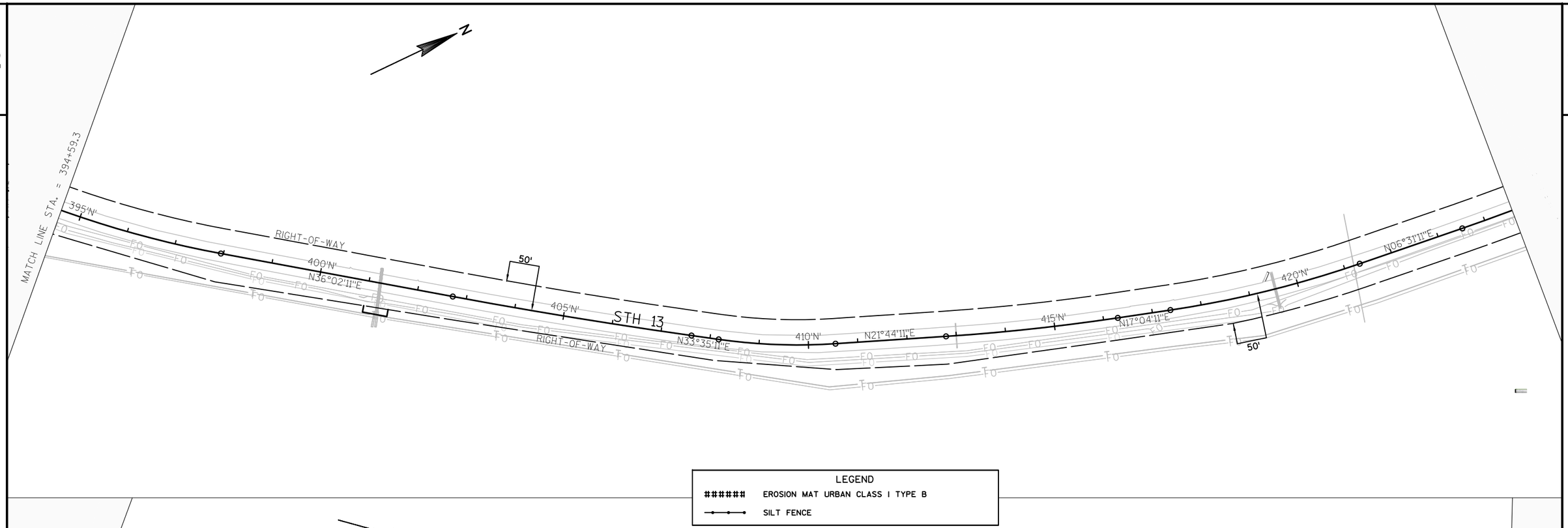
MATCH LINE STA. = 394+59.3

FO

TFO

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 15	E

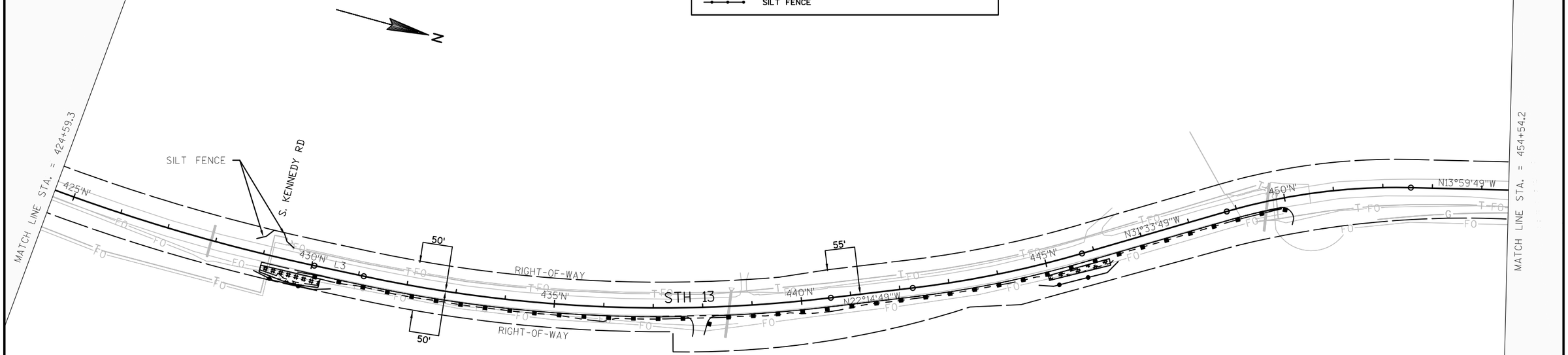
2



LEGEND

EROSION MAT URBAN CLASS I TYPE B

SILT FENCE



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 16	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\022001_EC_STH13.DWG
LAYOUT NAME - 022001_EC_STH13 - STH13_PN-13
PLOT DATE : 1/27/2017 10:12 AM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT

2

EXCEPTION TO NET ℄ LENGTH
STA. 470+71.9'N' - STA. 472+30.6'N'
STRUCTURE B-4-009 (PIKES CREEK)

N

SKI HILL RD

55'

460'N'

STH 13

465'N'

N20°24'11"E

65'

55'

475'N'

N33°28'11"E

50'

MILLIGAN RD

HATCHERY RD

480'N'

RIGHT-OF-WAY

N32°41'11"E

485'N'

455'N'

RIGHT-OF-WAY

MATCH LINE STA. = 454+54.2

MATCH LINE STA. = 485+46.8

LEGEND

EROSION MAT URBAN CLASS I TYPE B

SILT FENCE

N

MATCH LINE STA. = 485+46.8

50'

490'N'

RIGHT-OF-WAY

N32°41'11"E

65'

60'

495'N'

PORT SUPERIOR RD

500'N'

STH 13

80'

505'N'

N56°05'11"E

70'

62'

510'N'

60'

515'N'

BAYVIEW RD

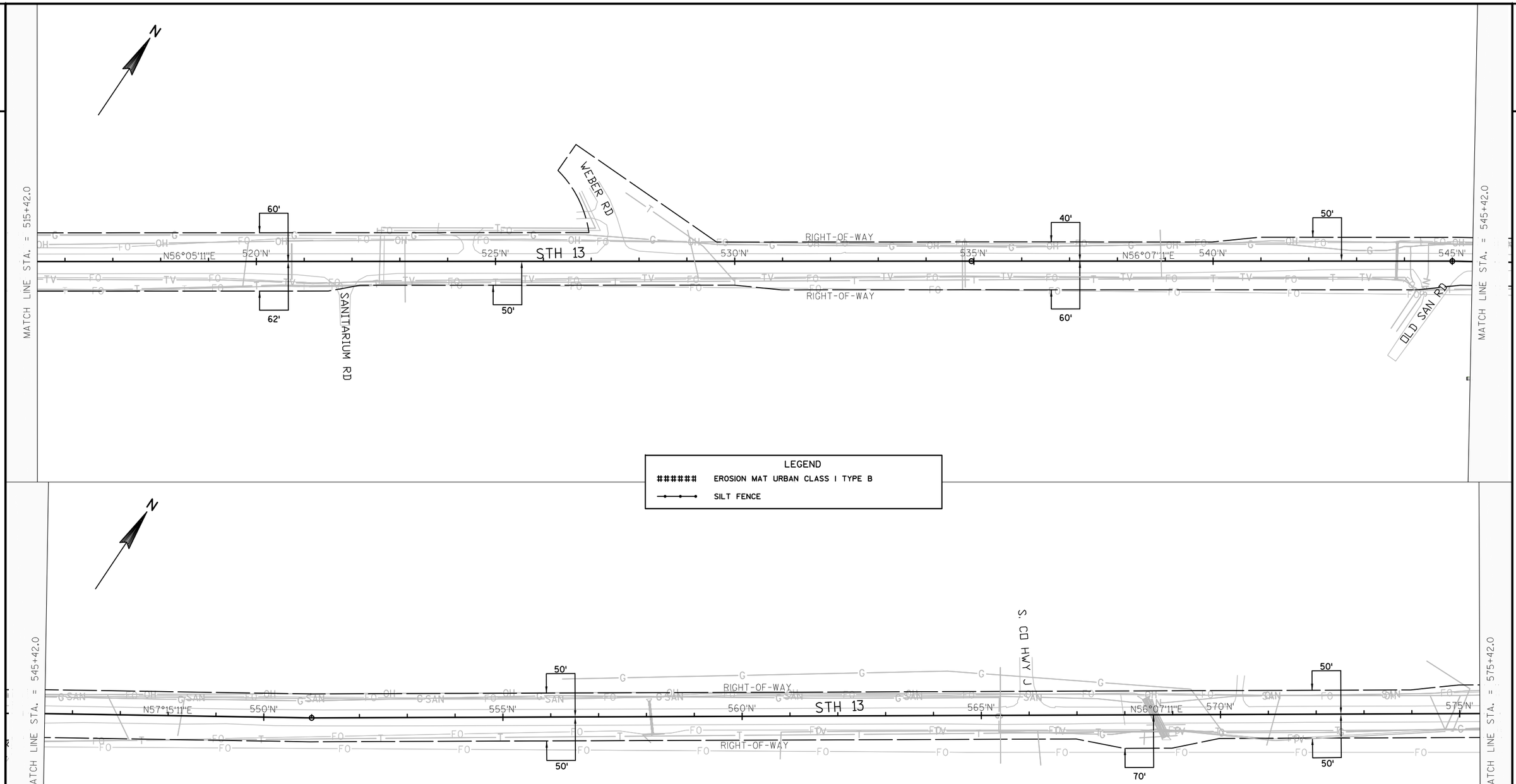
MATCH LINE STA. = 515+42.0

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 17	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\022001_EC_STH13.DWG
LAYOUT NAME - 022001_EC_STH13 - STH13_PN-15

PLOT DATE : 1/27/2017 10:12 AM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT

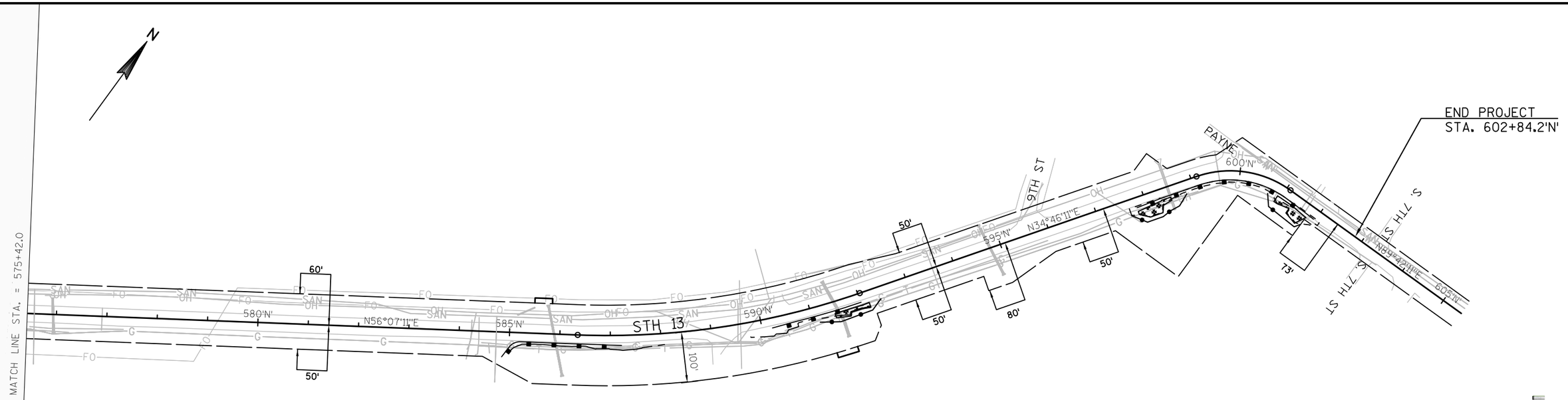
2



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 18	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\022001_EC_STH13.DWG
LAYOUT NAME - 022001_EC_STH13 - STH13_PN-17
PLOT DATE : 1/27/2017 10:12 AM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT

2



LEGEND

####### EROSION MAT URBAN CLASS I TYPE B

SILT FENCE

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	EROSION CONTROL	SHEET 19	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\022001_EC_STH13.DWG PLOT DATE : 1/27/2017 10:13 AM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT
LAYOUT NAME - 022001_EC_STH13 - STH13_PN-19

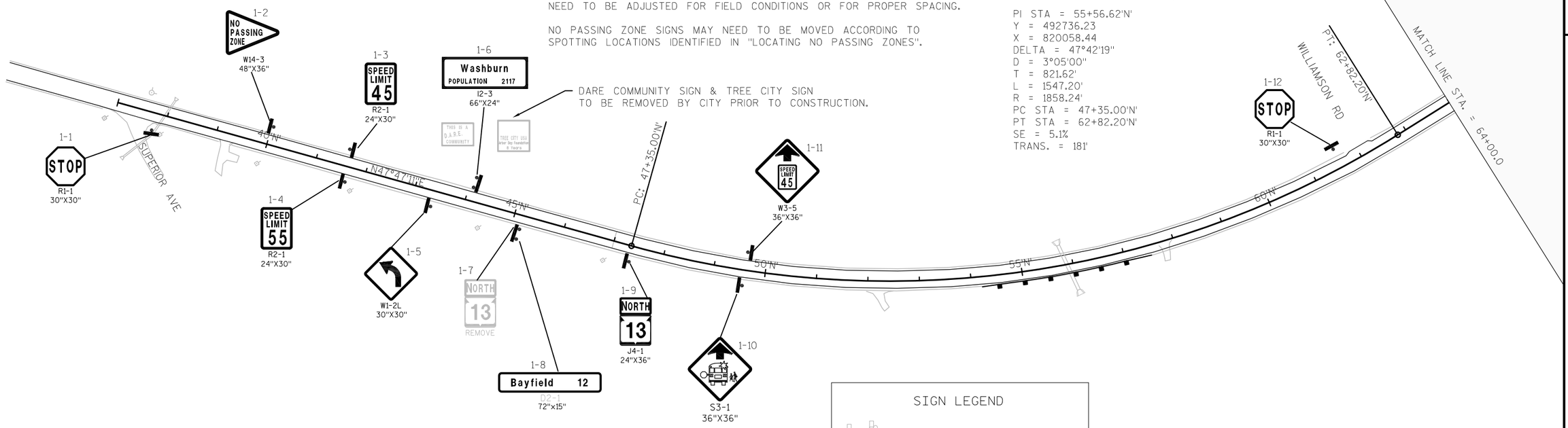
NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

PI STA = 55+56.62'N'
Y = 492736.23
X = 820058.44
DELTA = 47°42'19"
D = 3°05'00"
T = 821.62'
L = 1547.20'
R = 1858.24'
PC STA = 47+35.00'N'
PT STA = 62+82.20'N'
SE = 5.1%
TRANS. = 181'

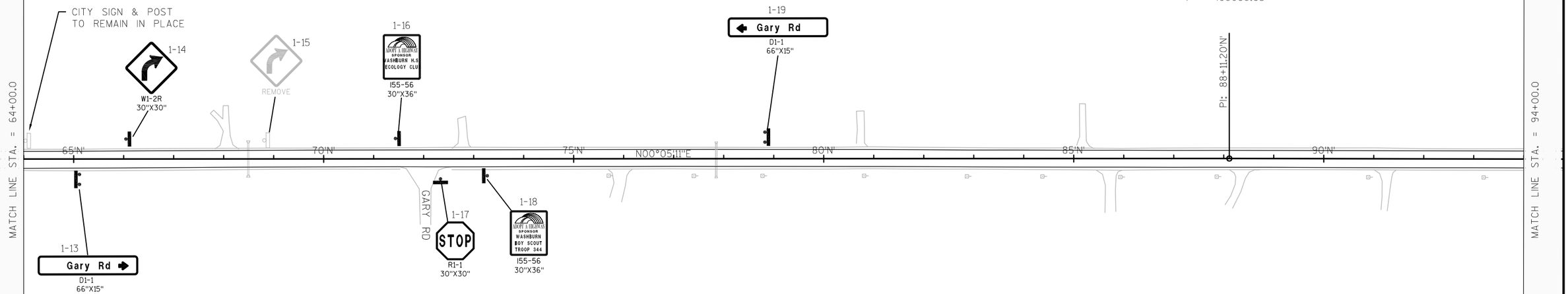


SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)

PI STA. 88+11.20
X = 820063.41
Y = 496086.85



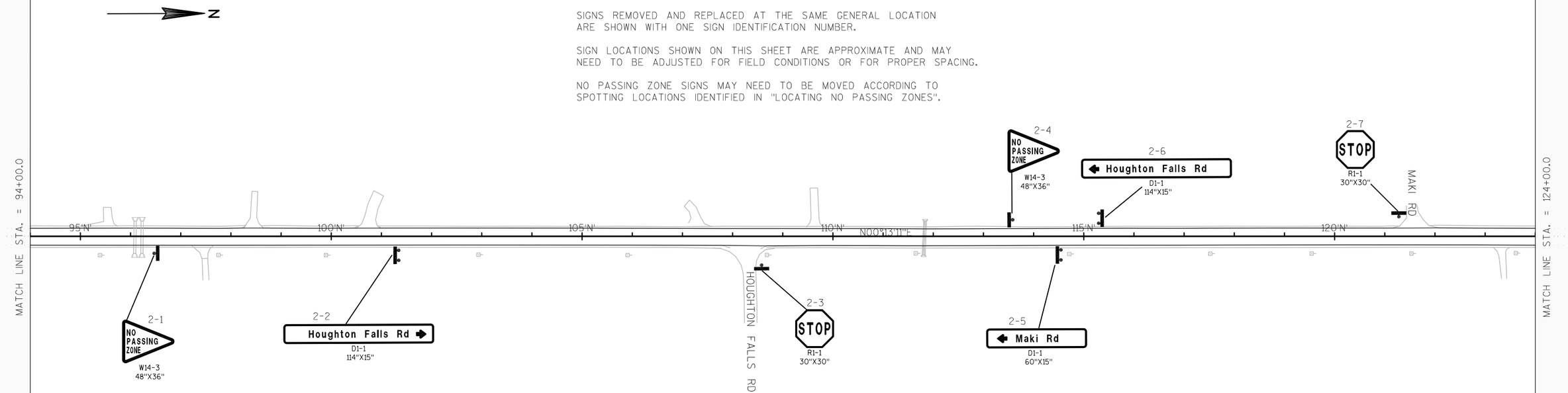
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 20	E

NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

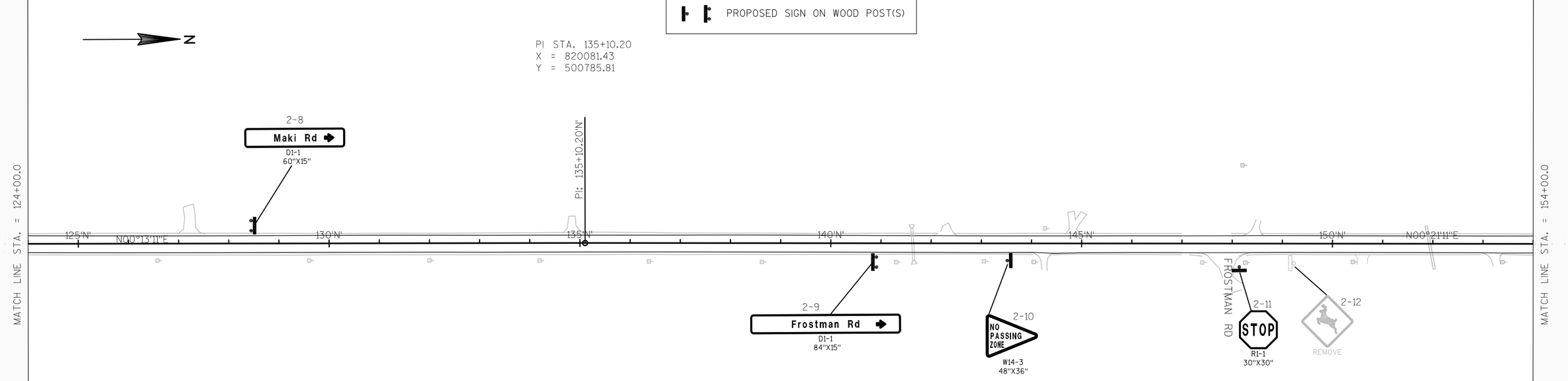


SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)

PI STA. 135+10.20
X = 820081.43
Y = 500785.81



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 21	E

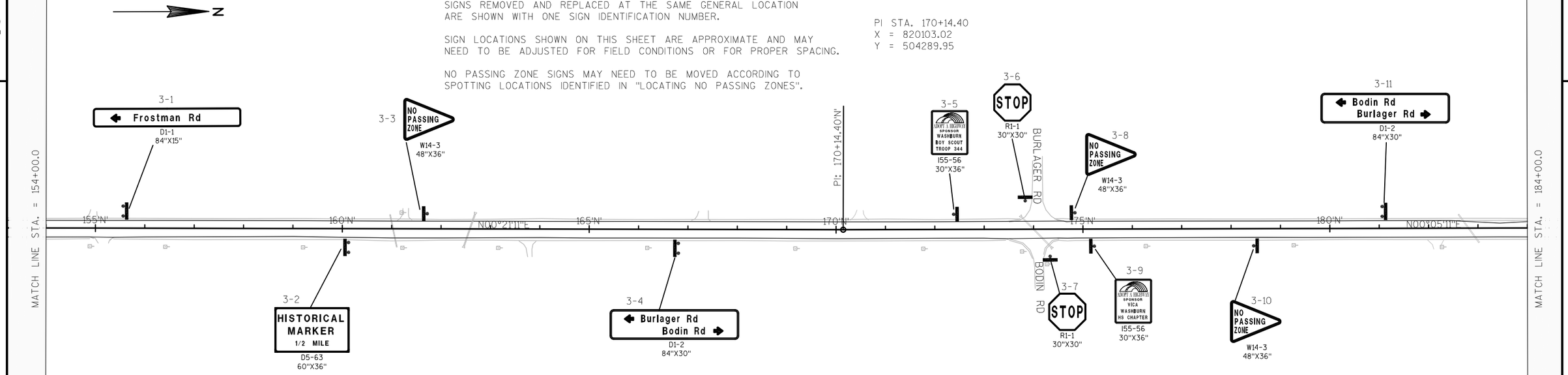
NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

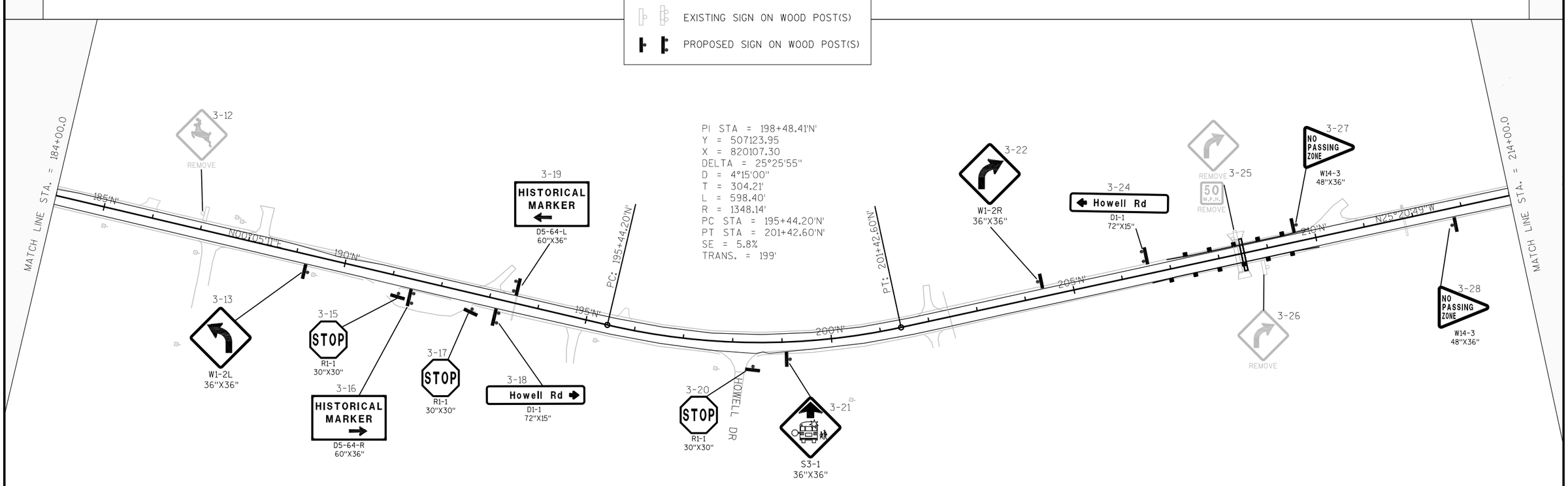
PI STA. 170+14.40
X = 820103.02
Y = 504289.95



SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 22	E

NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

MATCH LINE STA. = 214+00.0

PC: 214+51.30'N'

PT: 221+49.60'N'

MATCH LINE STA. = 244+00.6

4-1 HISTORICAL MARKER 1/2 MILE D5-63 60"X36"

4-2 NO PASSING ZONE W14-3 48"X36"

4-3 Friendly Valley Rd D1-61 102"X24"

215'N' 220'N' 225'N' 230'N' N16°36'49"W 235'N' 240'N'

STA BACK = 232+94.41'N'

STA AHEAD = 232+95.00'N'

PI STA = 218+01.13'N'
Y = 508897.75
X = 819267.06
DELTA = 8°43'44"
D = 1°15'00"
T = 349.83'
L = 698.30'
R = 4583.66'
PC STA = 214+51.30'N'
PT STA = 221+49.60'N'
SE = 2.8%
TRANS. = 122'

SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)

PI STA = 253+64.90'N'
Y = 512313.47
X = 818247.87
DELTA = 5°48'12"
D = 0°50'00"
T = 348.50'
L = 696.40'
R = 6875.49'
PC STA = 250+16.40'N'
PT STA = 257+12.80'N'
SE = 2.0%
TRANS. = 102'

MATCH LINE STA. = 244+00.6

MATCH LINE STA. = 274+00.6

4-4 NORTH 13 J4-1 24"X36"

4-5 SOUTH 13 J4-1 24"X36"

4-6 STOP R1-1 30"X30"

4-7 STOP R1-1 30"X30"

4-8 Kjarvick Rd D1-1 84"X15"

4-9 SPONSOR YICA WASHBURN HS CHAPTER I55-56 30"X36"

4-10 Friendly Valley Rd D1-61 102"X24"

4-11 SPONSOR SPINDRIFT CREW I55-56 30"X36"

4-12 STOP R1-1 30"X30"

4-13 Kjarvick Rd D1-1 84"X15"

FRIENDLY VALLEY RD

KJARVICK RD

PC: 250+16.40'N'

PT: 257+12.80'N'

245'N' N16°36'49"W 250'N' 255'N' 260'N' 265'N' N22°24'49"W 270'N'

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 23	E

NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

2

MATCH LINE STA. = 274+00.6

MATCH LINE STA. = 304+00.6

2

275'N 280'N 285'N N22°24'49"W 290'N 295'N 300'N

5-1 Bayview Park Rd D1-1 108"X15"

5-2 STOP R1-1 30"X30"

BAYVIEW PARK RD

5-3 Bayview Park Rd D1-1 108"X15"

SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)

5-4 McCulloch Rd D1-1 90"X15"

5-5 PASSING LANE AHEAD 1/2 MILE R4-51 36"X48"

5-6 NO PASSING ZONE W14-3 48"X36"

5-7 SPONSOR APOSTLE ISLANDS REALTY I55-56 30"X36"

5-8 SPONSOR SPINDRIFT CREW I55-56 30"X36"

5-9 STOP R1-1 30"X30"

MCCULLOCH RD

5-10 W1-2R 30"X30"

5-11 S3-1 36"X36"

5-12 McCulloch Rd D1-1 90"X15"

PC: 316+47.50'N

5-13 W5-52R 12"X36"

5-14 W5-52L 12"X36"

5-15 W5-52R 12"X36"

5-16 W5-52L 12"X36"

5-17 SLOWER TRAFFIC KEEP RIGHT R4-3 36"X48"

310'N 315'N 320'N 325'N 330'N

N22°24'49"W

PI STA = 327+35.85'N
Y = 519128.13
X = 815437.16
DELTA = 87°02'53"
D = 5°00'00"
T = 1088.35'
L = 1740.96'
R = 1145.92'
PC STA = 316+47.50'N
PT STA = 334+47.20'N
SE = 6.0%
TRANS. = 204'

PT: 334+47.20'N

STA BACK = 333+88.46'N
STA AHEAD = 334+47.20'N

MATCH LINE STA. = 334+76.8

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 24	E

2

N

MATCH LINE STA. = 334+76.8

PI STA = 354+73.10'N'
Y = 520462.72
X = 818250.94
DELTA = 4°26'48"
D = 0°45'00"
T = 296.60'
L = 592.90'
R = 7639.44'
PC STA = 351+76.50'N'
PT STA = 357+69.40'N'
SE = 2.0%
TRANS. = 102'

6-1 W1-2L 30"X30"

6-2 RIGHT LANE ENDS W9-1R 36"X36"

6-3 REMOVE

6-4 W4-2R 36"X36"

6-5 W4-2R 36"X36"

6-6 50 M.P.H. REMOVE

6-7 ONCOMING TRAFFIC USES CENTER LANE R3-72 30"X36"

6-8 W1-2L 30"X30"

335'N' 340'N' N64°37'11"E 345'N' 350'N' PC: 351+76.50'N' 355'N' PT: 357+69.40'N' 360'N' N69°04'11"E PC: 364+43.80'N'

NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)

PI STA = 366+27.90'N'
Y = 520875.37
X = 819329.82
DELTA = 14°38'53"
D = 4°00'00"
T = 184.10'
L = 366.20'
R = 1432.39'
PC STA = 364+43.80'N'
PT STA = 368+10.00'N'
SE = 5.7%
TRANS. = 197'

PI STA = 383+35.57'N'
Y = 521870.13
X = 820720.31
DELTA = 4°36'32"
D = 1°00'00"
T = 230.57'
L = 460.90'
R = 5729.58'
PC STA = 381+05.00'N'
PT STA = 385+65.90'N'
SE = 2.4%
TRANS. = 112'

PI STA = 391+11.37'N'
Y = 522370.86
X = 821313.20
DELTA = 3°49'55"
D = 1°00'00"
T = 191.67'
L = 383.20'
R = 5729.58'
PC STA = 389+19.70'N'
PT STA = 393+02.90'N'
SE = 2.4%
TRANS. = 112'

6-10 W1-2R 30"X30"

6-11 REMOVE

6-12 Whiting Rd D1-1 78"X15"

6-13 NO PASSING ZONE W14-3 48"X36"

6-14 STOP R1-1 30"X30" WHITING RD

6-15 NO PASSING ZONE W14-3 48"X36"

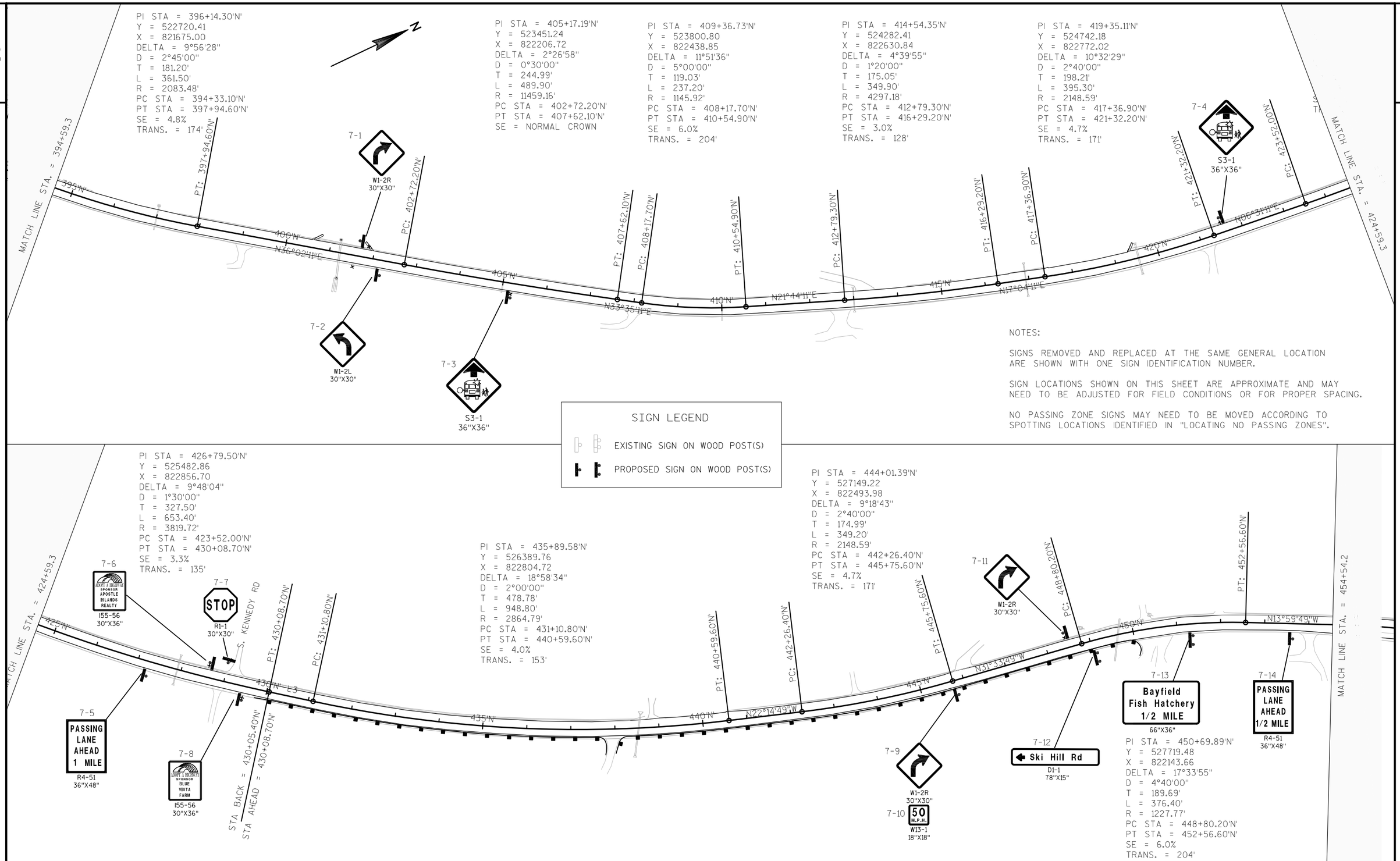
6-16 REMOVE

6-17 Whiting Rd D1-1 78"X15"

6-18 W1-2L 30"X30"

364+59.3 365'N' PT: 368+10.00'N' 370'N' N54°25'11"E 375'N' 380'N' PC: 381+05.00'N' 385'N' PT: 385+65.90'N' N49°49'11"E PC: 389+19.70'N' 390'N' PT: 393+02.90'N' PC: 394+33.10'N' N45°59'11"E MATCH LINE STA. = 394+59.3

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 25	E



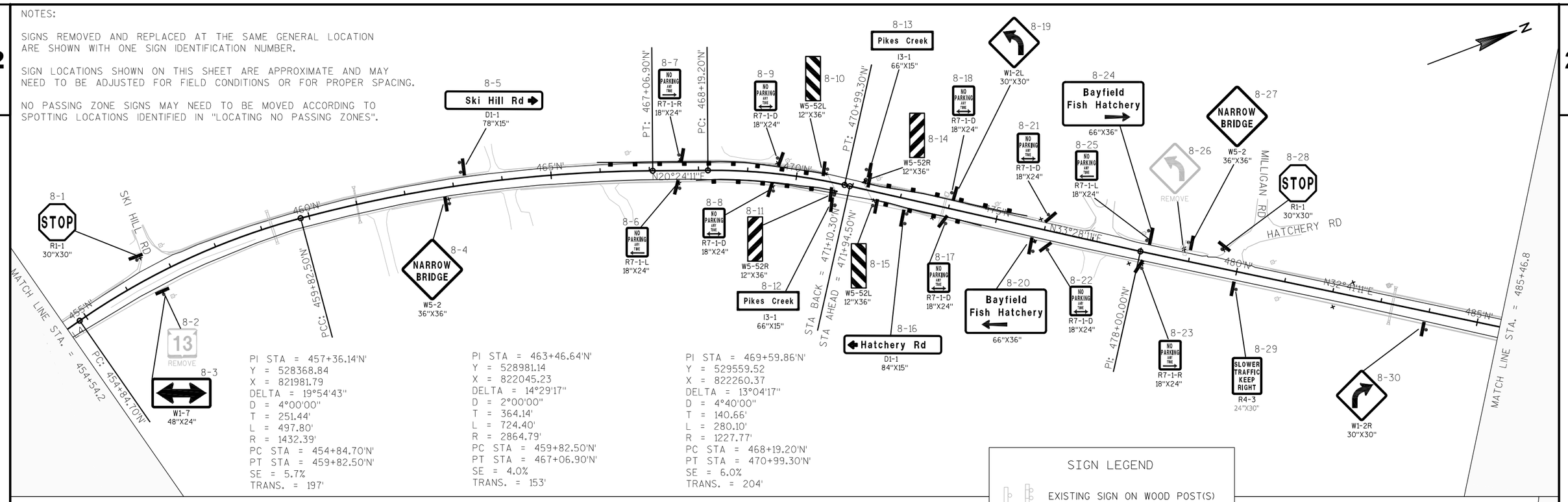
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 26	E

NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".



PI STA = 457+36.14'N'
Y = 528368.84
X = 821981.79
DELTA = 19°54'43"
D = 4°00'00"
T = 251.44'
L = 497.80'
R = 1432.39'
PC STA = 454+84.70'N'
PT STA = 459+82.50'N'
SE = 5.7%
TRANS. = 197'

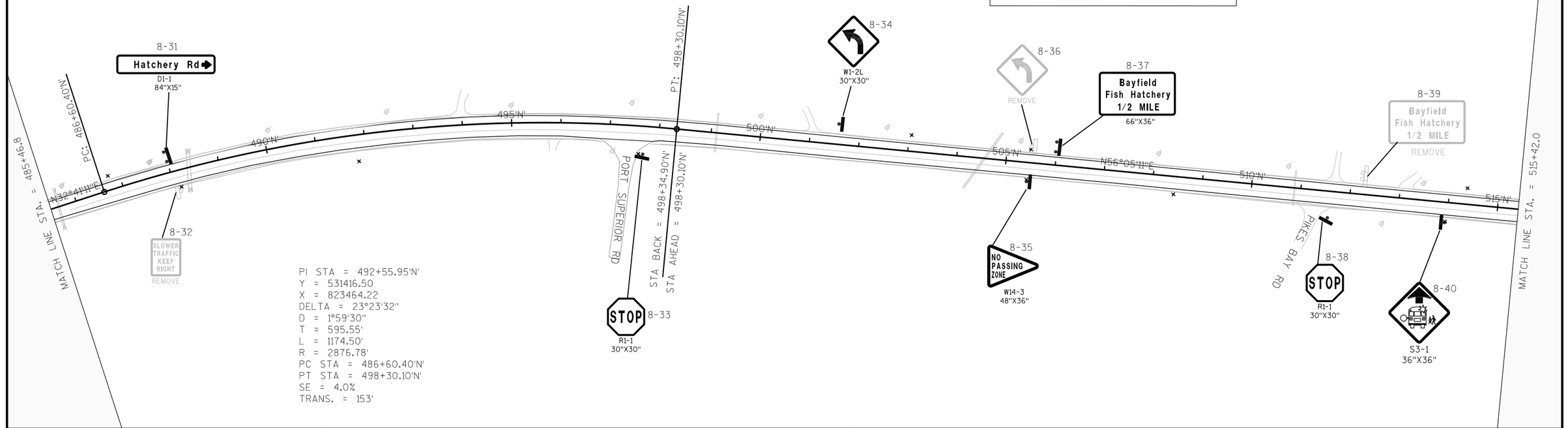
PI STA = 463+46.64'N'
Y = 528981.14
X = 822045.23
DELTA = 14°29'17"
D = 2°00'00"
T = 364.14'
L = 724.40'
R = 2864.79'
PC STA = 459+82.50'N'
PT STA = 467+06.90'N'
SE = 4.0%
TRANS. = 153'

PI STA = 469+59.86'N'
Y = 529559.52
X = 822260.37
DELTA = 13°04'17"
D = 4°40'00"
T = 140.66'
L = 280.10'
R = 1227.77'
PC STA = 468+19.20'N'
PT STA = 470+99.30'N'
SE = 6.0%
TRANS. = 204'

SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)



PI STA = 492+55.95'N'
Y = 531416.50
X = 823464.22
DELTA = 23°23'32"
D = 1°59'30"
T = 595.55'
L = 1174.50'
R = 2876.78'
PC STA = 486+60.40'N'
PT STA = 498+30.10'N'
SE = 4.0%
TRANS. = 153'

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 27	E

NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

2

MATCH LINE STA. = 515+42.0

N56°05'11"E

520'N'

525'N'

530'N'

535'N'

540'N'

545'N'

N56°07'11"E

PI: 534+94.83'N'

PI: 545+00.00'N'

9-1 (LIGHT 1, BEACON 11) SPONSOR BLUE VISTA FARM I55-56 30"X36"

9-3 ONCOMING TRAFFIC USES CENTER LANE REMOVE

9-5 STOP R1-1 30"X30"

WEBER RD

9-9 W4-2R 36"X36"

9-10 Weber Rd D1-1 72"X15"

9-12 ONCOMING TRAFFIC USES CENTER LANE R3-72 30"X36"

CHEQUAMEGON ROAD

9-2 STOP R1-1 30"X30"

9-4 Weber Rd D1-1 72"X15"

9-6 RIGHT LANE ENDS W9-1R 36"X36"

9-7 (LIGHT 1, BEACON 11) SPONSOR BAYFIELD HIGH SCHOOL CHOR SOCIETY I55-56 30"X36"

9-8 W4-2R 36"X36"

9-11 Old San Rd D1-1 78"X15"

OLD SAN RD

9-13 STOP R1-1 30"X30"

MATCH LINE STA. = 545+42.0

SIGN LEGEND

EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)

MATCH LINE STA. = 545+42.0

N57°15'11"E

550'N'

555'N'

560'N'

565'N'

570'N'

575'N'

PI: 551+00.12'N'

9-14 Old San Rd D1-1 78"X15"

9-15 SPEED LIMIT 45 W3-5 36"X36"

9-16 NO PASSING ZONE W14-3 48"X36"

9-17 NO PASSING ZONE W14-3 48"X36"

9-18 SPEED LIMIT 55 R2-1 24"X30"

9-19 SPEED LIMIT 45 R2-1 24"X30"

9-20 STOP R1-1 30"X30"

9-21 JCT COUNTY J J1-1 24"X39"

9-22 SPEED LIMIT 45 R2-1 24"X30"

9-23 SOUTH 13 J4-1 24"X36"

9-24 (LAKE SUPERIOR CIRCLE TOUR) M1-91 24"X24"

REMAIN INPLACE

9-25 COUNTY J JI3-1 24"X45"

9-26 STOP R1-1 36"X36"

S. CO HWY J

9-27 COUNTY J JI3-1 24"X45"

TOURIST ORIENTED DIRECTIONAL SIGN TO REMAIN IN PLACE

9-28 NORTH 13 J4-1 24"X36"

9-29 (LAKE SUPERIOR CIRCLE TOUR) M1-91 24"X24"

REMAIN INPLACE

9-30 SPEED LIMIT 45 R2-1 24"X30"

9-31 JCT COUNTY J J1-1 24"X39"

9-32 SPEED LIMIT 45 R2-1 24"X30"

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 28	E

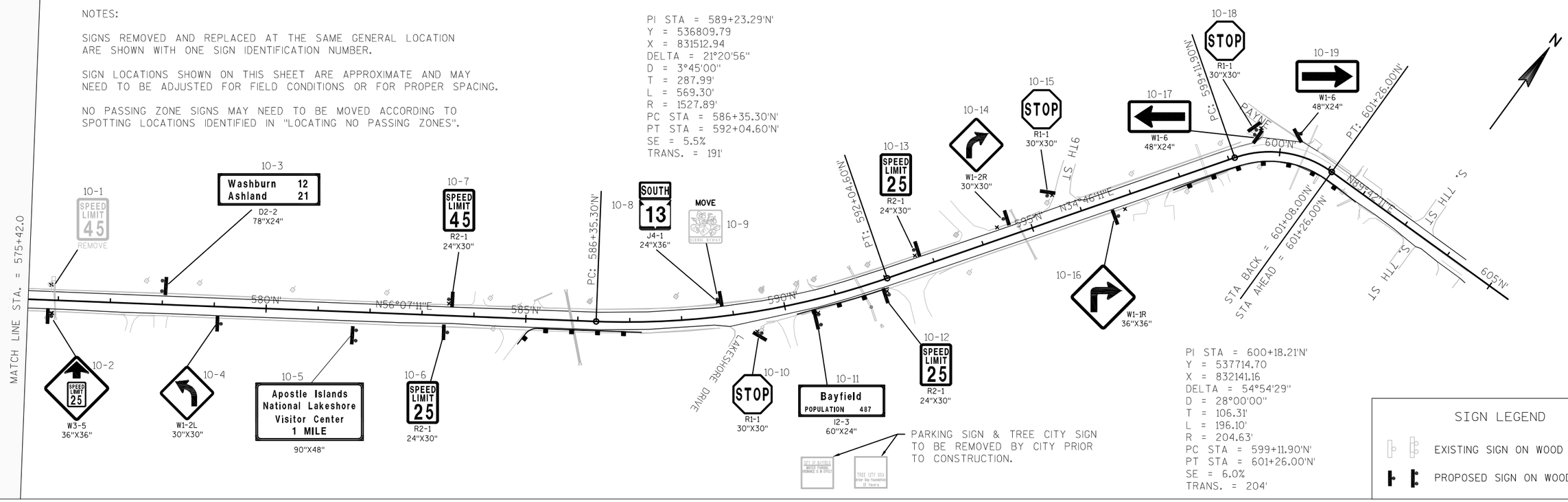
NOTES:

SIGNS REMOVED AND REPLACED AT THE SAME GENERAL LOCATION ARE SHOWN WITH ONE SIGN IDENTIFICATION NUMBER.

SIGN LOCATIONS SHOWN ON THIS SHEET ARE APPROXIMATE AND MAY NEED TO BE ADJUSTED FOR FIELD CONDITIONS OR FOR PROPER SPACING.

NO PASSING ZONE SIGNS MAY NEED TO BE MOVED ACCORDING TO SPOTTING LOCATIONS IDENTIFIED IN "LOCATING NO PASSING ZONES".

PI STA = 589+23.29'N'
Y = 536809.79
X = 831512.94
DELTA = 21°20'56"
D = 3°45'00"
T = 287.99'
L = 569.30'
R = 1527.89'
PC STA = 586+35.30'N'
PT STA = 592+04.60'N'
SE = 5.5%
TRANS. = 191'



PI STA = 600+18.21'N'
Y = 537714.70
X = 832141.16
DELTA = 54°54'29"
D = 28°00'00"
T = 106.31'
L = 196.10'
R = 204.63'
PC STA = 599+11.90'N'
PT STA = 601+26.00'N'
SE = 6.0%
TRANS. = 204'

SIGN LEGEND

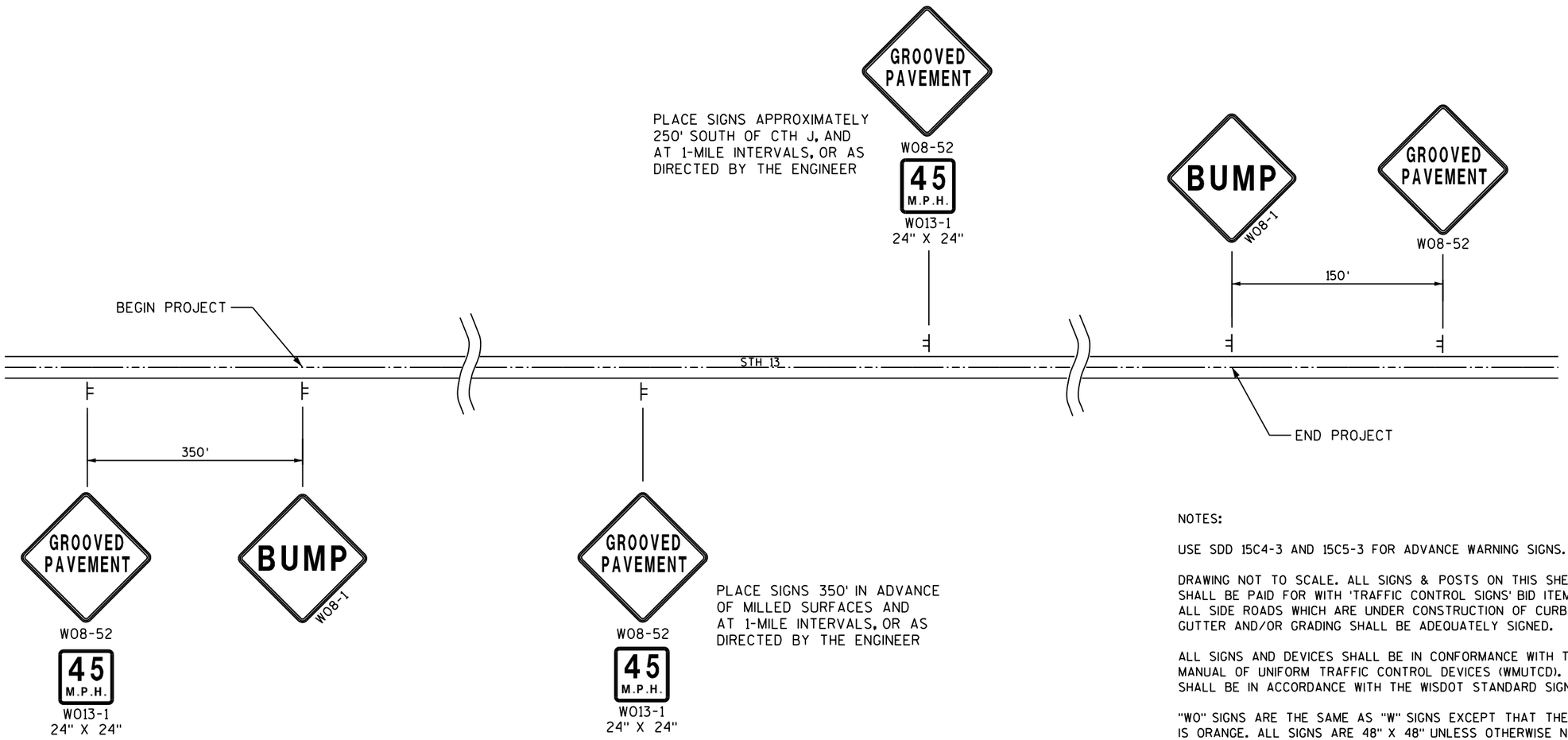
EXISTING SIGN ON WOOD POST(S)

PROPOSED SIGN ON WOOD POST(S)

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET 29	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\023201_PS_STH13.DWG
LAYOUT NAME - 023201_PS_STH13 - STH13_PN-19
PLOT DATE : 12/30/2016 4:15 PM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT

2



NOTES:

USE SDD 15C4-3 AND 15C5-3 FOR ADVANCE WARNING SIGNS.

DRAWING NOT TO SCALE. ALL SIGNS & POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB & GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE. ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

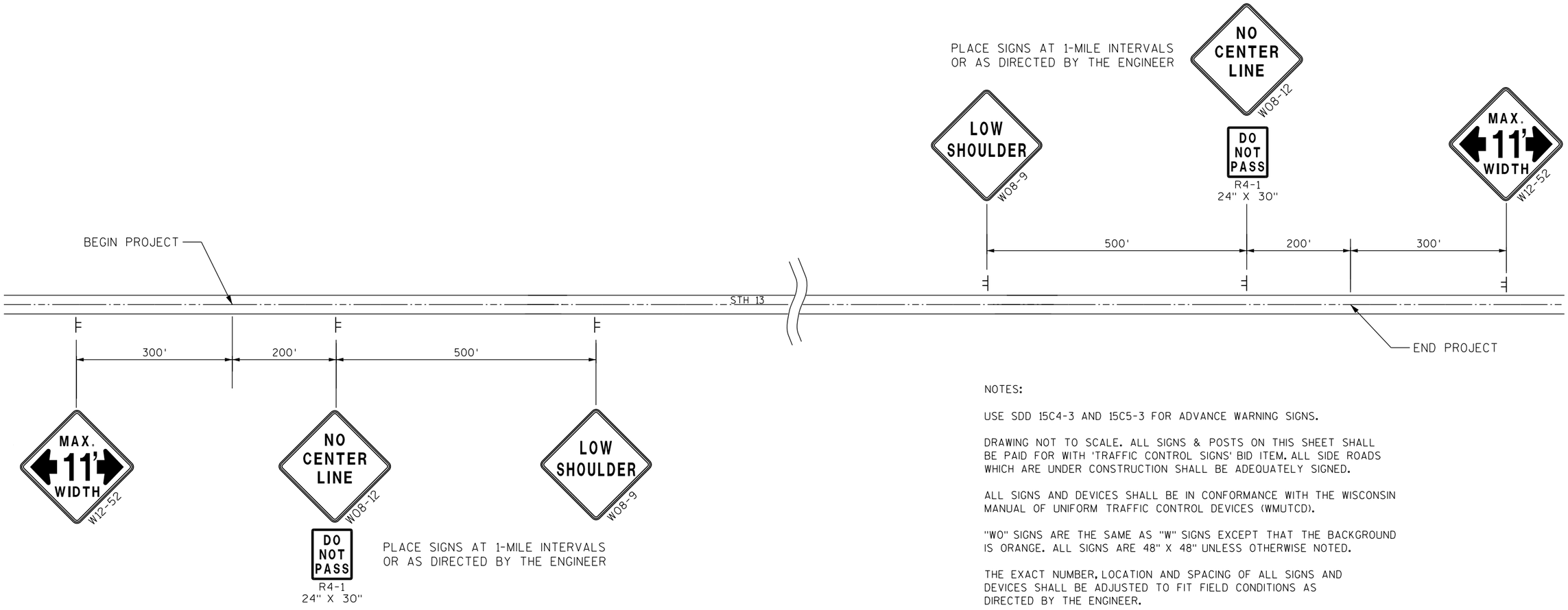
THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE NEXT DETAIL SHEET FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MARKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

DETAIL FOR SIGNING ON MAINLINE MILLED SURFACES

2



PLACE SIGNS AT 1-MILE INTERVALS
OR AS DIRECTED BY THE ENGINEER

NOTES:

USE SDD 15C4-3 AND 15C5-3 FOR ADVANCE WARNING SIGNS.

DRAWING NOT TO SCALE. ALL SIGNS & POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE. ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

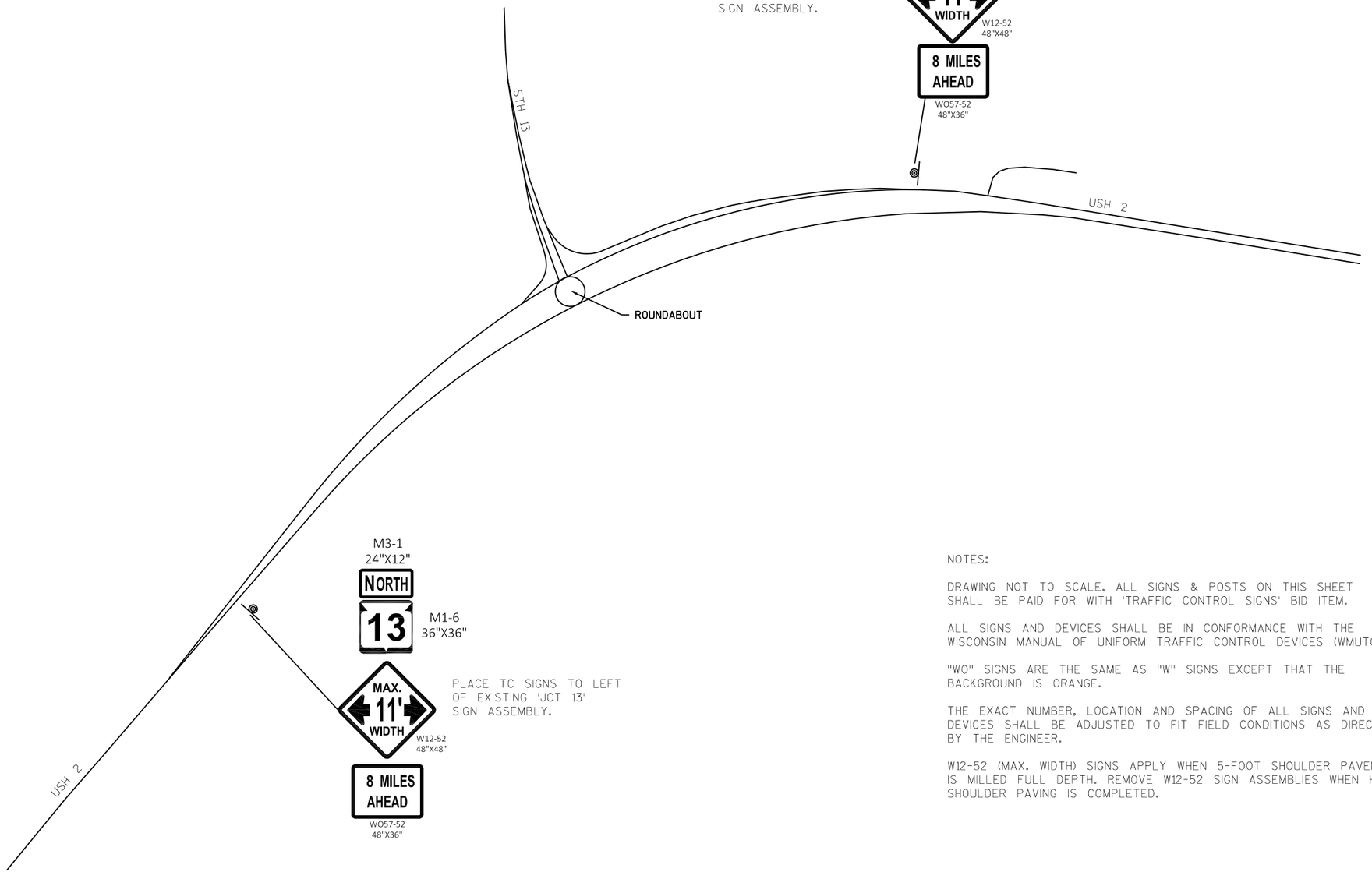
REMOVE OR COVER WO8-12 AND R4-1 SIGNS IMMEDIATELY FOLLOWING PLACEMENT OF TEMPORARY OR SAME-DAY PAVEMENT MARKINGS.

W12-52 (MAX. WIDTH) SIGNS APPLY WHEN 5-FOOT SHOULDER PAVEMENT IS MILLED FULL DEPTH. REMOVE W12-52 AND WO8-9 SIGNS WHEN HMA SHOULDER PAVING IS COMPLETED.

DETAIL FOR SIGNING DURING SHOULDER WORK

2

N



NOTES:

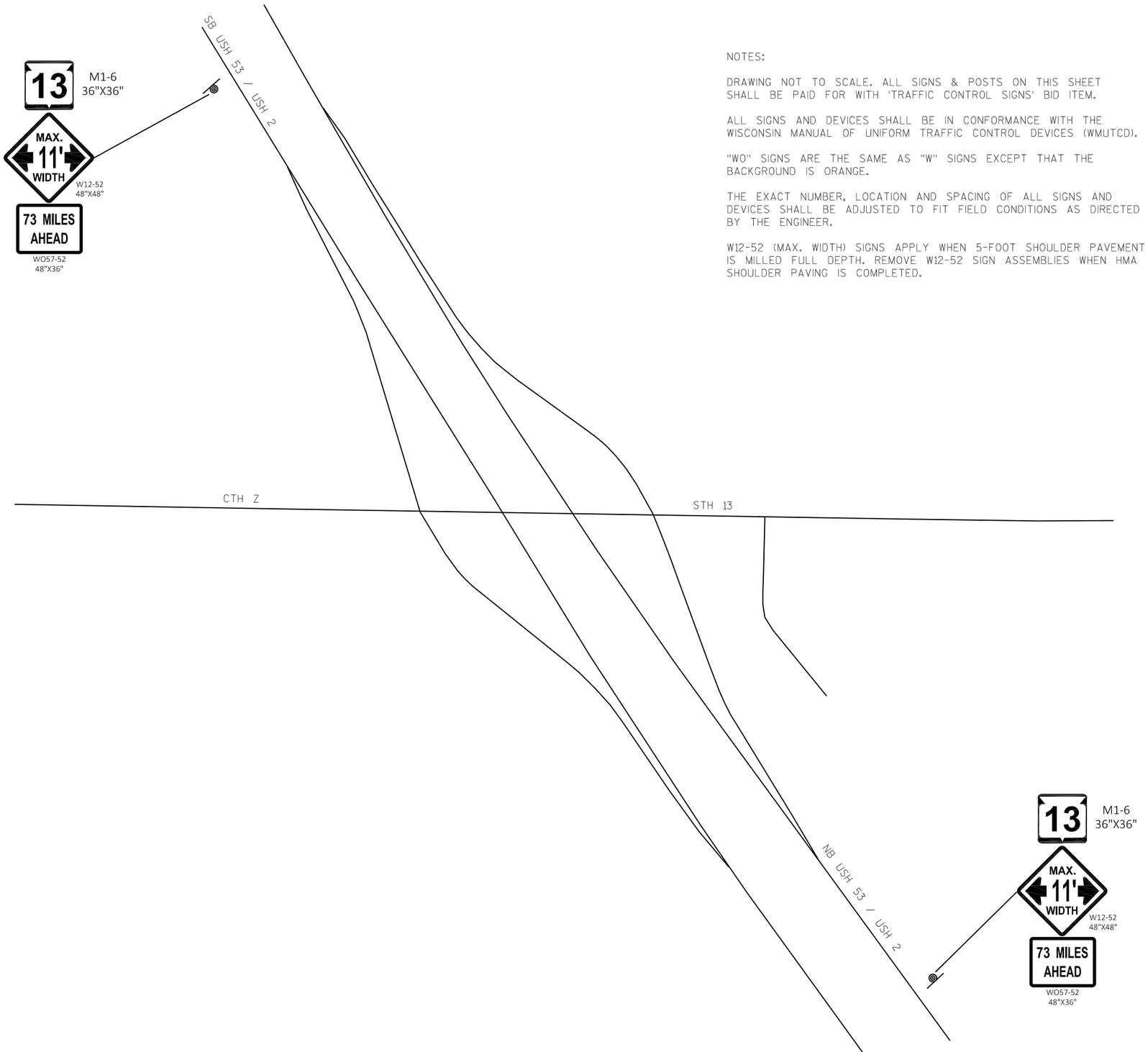
DRAWING NOT TO SCALE. ALL SIGNS & POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

W12-52 (MAX. WIDTH) SIGNS APPLY WHEN 5-FOOT SHOULDER PAVEMENT IS MILLED FULL DEPTH. REMOVE W12-52 SIGN ASSEMBLIES WHEN HMA SHOULDER PAVING IS COMPLETED.



NOTES:

DRAWING NOT TO SCALE. ALL SIGNS & POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD).

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

W12-52 (MAX. WIDTH) SIGNS APPLY WHEN 5-FOOT SHOULDER PAVEMENT IS MILLED FULL DEPTH. REMOVE W12-52 SIGN ASSEMBLIES WHEN HMA SHOULDER PAVING IS COMPLETED.

N

2

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	TRAFFIC CONTROL ADVANCE SIGNING (WEST)	SHEET 33	E

8160-03-70

Line	Item	Item Description	Unit	Total	Qty
0010	202.0105	Roadside Clearing	STA	269.000	269.000
0020	204.0110	Removing Asphaltic Surface	SY	1,941.000	1,941.000
0030	204.0115	Removing Asphaltic Surface Butt Joints	SY	2,236.000	2,236.000
0040	204.0120	Removing Asphaltic Surface Milling	SY	169,289.000	169,289.000
0050	204.0165	Removing Guardrail	LF	2,268.000	2,268.000
0060	205.9015.S	Grading Shaping and Finishing Intersection (location) 01. S. Kennedy Rd	LS	1.000	1.000
0070	211.0100	Prepare Foundation for Asphaltic Paving (project) 01. 8160-03-70	LS	1.000	1.000
0080	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	1,017.000	1,017.000
0090	213.0100	Finishing Roadway (project) 01. 8160-03-70	EACH	1.000	1.000
0100	305.0110	Base Aggregate Dense 3/4-Inch	TON	6,064.000	6,064.000
0110	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,044.000	1,044.000
0120	305.0500	Shaping Shoulders	STA	1,046.000	1,046.000
0130	440.4410	Incentive IRI Ride	DOL	42,880.000	42,880.000
0140	455.0605	Tack Coat	GAL	23,663.000	23,663.000
0150	460.2000	Incentive Density HMA Pavement	DOL	19,420.000	19,420.000
0160	460.4110.S	Reheating HMA Pavement Longitudinal Joints	LF	63,060.000	63,060.000
0170	460.6224	HMA Pavement 4 MT 58-28 S	TON	22,135.000	22,135.000
0180	460.6244	HMA Pavement 4 MT 58-34 S	TON	6,704.000	6,704.000
0190	460.8444	HMA Pavement 4 SMA 58-34 H	TON	15,342.000	15,342.000
0200	465.0105	Asphaltic Surface	TON	4,000.000	4,000.000
0210	465.0110	Asphaltic Surface Patching	TON	250.000	250.000
0220	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	350.000	350.000
0230	465.0310	Asphaltic Curb	LF	248.000	248.000
0240	465.0425	Asphaltic Shoulder Rumble Strips 2-Lane Rural	LF	81,330.000	81,330.000
0250	465.0475	Asphalt Center Line Rumble Strips 2-Lane Rural	LF	44,401.000	44,401.000
0260	614.0010	Barrier System Grading Shaping Finishing	EACH	12.000	12.000
0270	614.0305	Steel Plate Beam Guard Class A	LF	2,025.000	2,025.000
0280	614.0345	Steel Plate Beam Guard Short Radius	LF	175.000	175.000
0290	614.0370	Steel Plate Beam Guard Energy Absorbing Terminal	EACH	2.000	2.000
0300	614.0390	Steel Plate Beam Guard Short Radius Terminal	EACH	4.000	4.000
0310	614.2300	MGS Guardrail 3	LF	3,075.000	3,075.000
0320	614.2330	MGS Guardrail 3 K	LF	712.500	712.500
0330	614.2340	MGS Guardrail 3 L	LF	325.000	325.000
0340	614.2500	MGS Thrie Beam Transition	LF	472.800	472.800
0350	614.2610	MGS Guardrail Terminal EAT	EACH	21.000	21.000
0360	618.0100	Maintenance And Repair of Haul Roads (project) 01. 8160-03-70	EACH	1.000	1.000
0370	619.1000	Mobilization	EACH	1.000	1.000

8160-03-70

Line	Item	Item Description	Unit	Total	Qty
0380	628.1504	Silt Fence	LF	7,902.000	7,902.000
0390	628.1520	Silt Fence Maintenance	LF	7,902.000	7,902.000
0400	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0410	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0420	628.2008	Erosion Mat Urban Class I Type B	SY	6,347.000	6,347.000
0430	633.0100	Delineator Posts Steel	EACH	100.000	100.000
0440	633.0500	Delineator Reflectors	EACH	100.000	100.000
0450	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	47.000	47.000
0460	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	185.000	185.000
0470	637.2210	Signs Type II Reflective H	SF	973.220	973.220
0480	637.2230	Signs Type II Reflective F	SF	426.250	426.250
0490	638.2102	Moving Signs Type II	EACH	1.000	1.000
0500	638.2602	Removing Signs Type II	EACH	186.000	186.000
0510	638.3000	Removing Small Sign Supports	EACH	227.000	227.000
0520	642.5001	Field Office Type B	EACH	1.000	1.000
0530	643.0100	Traffic Control (project) 01. 8160-03-70	EACH	1.000	1.000
0540	643.0300	Traffic Control Drums	DAY	3,618.000	3,618.000
0550	643.0705	Traffic Control Warning Lights Type A	DAY	3,618.000	3,618.000
0560	643.0900	Traffic Control Signs	DAY	4,534.000	4,534.000
0570	643.1050	Traffic Control Signs PCMS	DAY	194.000	194.000
0580	646.0106	Pavement Marking Epoxy 4-Inch	LF	80,572.000	80,572.000
0590	646.0842.S	Pavement Marking Grooved Contrast Wet Reflective Epoxy 4-Inch	LF	1,631.000	1,631.000
0600	646.2304.S	Pavement Marking Grooved Wet Reflective Epoxy 4-Inch	LF	114,778.000	114,778.000
0610	648.0100	Locating No-Passing Zones	MI	10.720	10.720
0620	649.0402	Temporary Pavement Marking Paint 4-Inch	LF	229,644.000	229,644.000
0630	650.8000	Construction Staking Resurfacing Reference	LF	56,123.000	56,123.000
0640	650.9910	Construction Staking Supplemental Control (project) 01. 8160-03-70	LS	1.000	1.000
0650	690.0150	Sawing Asphalt	LF	880.000	880.000
0660	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	2,400.000	2,400.000
0670	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	990.000	990.000
0680	SPV.0105	Special 01. Material Transfer Vehicle	LS	1.000	1.000
0690	SPV.0105	Special 02. Milling and Removing Temporary Joint	LS	1.000	1.000
0700	SPV.0180	Special 01. Removing Asphaltic Surface Milling Full Depth	SY	62,227.000	62,227.000
0710	SPV.0195	Special 01. SMA Pavement Compaction Acceptance	TON	15,342.000	15,342.000

REMOVING ASPHALTIC SURFACE BUTT JOINTS

STATION	-	STATION	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY
CAT. 0010				
37+00	-	37+50	STH 13	189
231+68	-	232+18	STH 13	189
233+74	-	234+24	STH 13	189
238+39	-	238+89	STH 13	189
240+46	-	240+96	STH 13	189
319+69	-	320+19	STH 13	189
321+03	-	321+53	STH 13	189
470+22	-	470+72	STH 13	189
472+31	-	472+81	STH 13	189
602+34	-	602+84	STH 13	189
			SUPERIOR AVE	27
			BURLAGER RD	26
			FRIENDLY VALLEY RD	27
			SKI HILL RD	29
			HATCHERY RD	27
			PORT SUPERIOR RD	34
			CHEQUAMEGON RD	26
			WEBER RD	30
			OLD SAN RD	30
			CTH J	33
			9TH ST	32
			PAYNE AVE	25
			TOTAL	2236

PREPARE FOUNDATION FOR ASPHALTIC PAVING

LOCATION	211.0100 PREPARE FOUNDATION FOR ASPHALTIC PAVING 8160-03-70 LS
CAT 0010	
STH 13	1
PROJECT TOTAL	1

PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS

STATION	-	STATION	LOCATION	211.0400 PREP. FOUNDATION FOR ASPHALTIC SHOULDERS STA
CAT. 0010				
37+00	-	228+04	13'N' RT & LT	383
228+04	-	230+04	13'N' LT	2
234+75	-	237+89	13'N' RT & LT	7
241+50	-	319+12	13'N' RT & LT	156
322+12	-	330+85	13'N' RT & LT	18
330+85	-	357+70	13'N' LT	27
357+70	-	470+72	13'N' RT & LT	227
472+31	-	479+00	13'N' RT & LT	14
479+00	-	544+00	13'N' LT	65
544+00	-	602+84	13'N' RT & LT	118
			TOTAL	1017

REMOVING ASPHALTIC SURFACE MILLING

STATION	-	STATION	LOCATION	204.0120 REMOVING ASPHALTIC SURFACE MILLINIG SY
CAT. 0010				
37+50	-	602+34	STH 13	163251
			SUPERIOR AVE	152
			WILLIAMS RD	162
			GARY RD	237
			HOUGHTON FALLS RD	234
			MAKI RD	186
			FROSTMAN RD	187
			BURLAGER RD	169
			BODIN RD	192
			HOWELL DR	213
			SIOUX RIVER HEIGHTS RD	206
			FRIENDLY VALLEY RD W	205
			FRIENDLY VALLEY RD E	210
			KJARVICK RD	317
			BAYVIEW PARK RD	196
			MCCULLOCH RD	156
			WHITING RD	406
			CARRIER RD	84
			S. KENNEDY RD	84
			SKI HILL RD	238
			HATCHERY RD	146
			PORT SUPERIOR RD	278
			PIKES BAY RD	168
			CHEQUAMEGON RD	180
			WEBER RD	187
			OLD SAN RD	504
			CTH J	185
			LAKESHORE DR	206
			9TH ST	130
			PAYNE AVE	220
			TOTAL	169289

SHAPING SHOULDERS

STATION	-	STATION	LOCATION		305.0500 SHAPING SHOULDERS STA
CAT. 0010					
37+00	-	228+40	STH 13	LT	192
250+00	-	371+13	STH 13	LT	122
379+00	-	379+89	STH 13	LT	1
382+14		384+14	STH 13	LT	2
389+19		393+35	STH 13	LT	5
395+48		397+48	STH 13	LT	2
411+12	-	414+22	STH 13	LT	4
415+38		417+50	STH 13	LT	3
419+26		439+68	STH 13	LT	21
422+00	-	439+00	STH 13	LT	17
440+56	-	466+64	STH 13	LT	27
476+78	-	532+50	STH 13	LT	56
535+01	-	599+50	STH 13	LT	65
37+00	-	231+26	STH 13	RT	195
246+24	-	355+44	STH 13	RT	110
362+01	-	466+66	STH 13	RT	105
475+92	-	589+75	STH 13	RT	114
592+16	-	596+57	STH 13	RT	5
			TOTAL		1046

ROADSIDE CLEARING

STATION	-	STATION	LOCATION	202.0105 ROADSIDE CLEARING STA	CLEARING LIMITS LT	CLEARING LIMITS RT	EXIST. RW LT	EXIST. RW RT	COMMENT OR "INCLUDES... "
CAT 0010									
37+00	-	48+00	LT	11	45'		50'		LEFT SIDE ONLY
50+00	-	51+00	LT & RT	1	45'	45'	50'	50'	BOTH SIDES
53+00	-	57+00	LT & RT	4	45'	45'	50'	50'	BOTH SIDES
95+00	-	97+00	LT	2	45'		50'		LEFT SIDE ONLY
98+00	-	99+00	RT	1		45'		50'	RIGHT SIDE ONLY
101+00	-	107+00	LT	6	45'		61'		LEFT SIDE ONLY
118+00	-	122+00	LT	4	45'		50'		LEFT SIDE ONLY
140+00	-	161+00	LT	21	45'	45'	50'	50'	141+00 - 143+00 (RT)
167+00	-	169+00	LT	2	45'		50'		LEFT SIDE ONLY
185+00	-	188+00	LT	3	45'		50'		LEFT SIDE ONLY
195+00	-	196+00	LT	1	45'		50		LEFT SIDE ONLY
203+00	-	227+00	LT	24	45'	45'	60'	50'	206+00 - 207+00 (RT)
243+00	-	250+00	LT	7	45'		50'		LEFT SIDE ONLY
283+00	-	286+00	LT & RT	3	45'	36'	50'	41'	BOTH SIDES-REDUCED R/W RT
286+00	-	309+00	LT	23	45		50'		LEFT SIDE ONLY
360+00	-	376+00	RT	16	45'	45'	56'	65'	363+00 -373+00 (LT)
377+00	-	424+00	LT	47	45'	42'	56'	47'	393+00 -424+00 (RT) *
424+00	-	429+00	RT	5		42'		47'	RIGHT SIDE ONLY
429+00	-	456+00	LT	27	45'	45'	50'	50'	442+00 - 448+00 (RT)
457+00	-	466+00	LT	9	45'		50'		LEFT SIDE ONLY
464+00	-	467+00	RT	3		35'		43'	RIGHT SIDE ONLY
496+00	-	499+00	LT	3	45'		60'		LEFT SIDE ONLY
502+00	-	510+00	RT	8		45'		60'	RIGHT SIDE ONLY
550+00	-	565+00	RT	15		45'		50'	RIGHT SIDE ONLY
568+00	-	570+00	RT	2		45'		50'	RIGHT SIDE ONLY
578+00	-	589+00	RT	11		45'		50'->50'	RIGHT SIDE ONLY
				10					UNDISTRIBUTED
			TOTAL	269					

* INTERMITTENT LOCATIONS

BASE AGGREGATE DENSE 1 1/4-INCH

STATION	-	STATION	LOCATION		305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	COMMENTS
CAT. 0010						
53+02	-	57+52	STH 13	RT	33	BARRIER SYSTEM
205+83	-	210+00	STH 13	LT	31	BARRIER SYSTEM
206+00	-	211+00	STH 13	RT	31	BARRIER SYSTEM
228+40	-	250+00	STH 13	LT	55	BARRIER SYSTEM
231+26	-	246+24	STH 13	RT	119	BARRIER SYSTEM
355+44	-	362+01	STH 13	RT	41	BARRIER SYSTEM
429+00	-	450+06	STH 13	RT	18	BARRIER SYSTEM
464+10	-	476+78	STH 13	LT	41	BARRIER SYSTEM
466+66	-	475+92	STH 13	RT	37	BARRIER SYSTEM
585+01	-	588+77	STH 13	RT	20	BARRIER SYSTEM
589+75	-	592+16	STH 13	RT	23	BARRIER SYSTEM
597+65	-	602+10	STH 13	RT	47	BARRIER SYSTEM
429+20			S. KENNEDY RD APRON		48	ASPHALT APRON BASE
PROJECT			UNDISTRIBUTED		500	SHDR. PREPARATION
			TOTAL		1044	

BASE AGGREGATE DENSE 3/4-INCH

STATION		STATION	LOCATION		305.0110 * BASE AGGREGATE DENSE 3/4-INCH TON	COMMENTS
CAT. 0010						
37+00	-	205+83	STH 13	LT	750	SHDR. GRAVEL
205+83	-	210+00	STH 13	LT	21	BARRIER SYSTEM
210+00		228+40	STH 13	LT	82	SHDR. GRAVEL
228+40	-	250+00	STH 13	LT	42	BARRIER SYSTEM
250+00	-	371+13	STH 13	LT	538	SHDR. GRAVEL
391+00	-	398+00	STH 13	LT	31	SHDR. GRAVEL
422+00	-	439+00	STH 13	LT	76	SHDR. GRAVEL
440+50	-	464+10	STH 13	LT	105	SHDR. GRAVEL
464+10	-	476+78	STH 13	LT	33	BARRIER SYSTEM
476+78	-	532+31	STH 13	LT	247	SHDR. GRAVEL
535+01	-	599+50	STH 13	LT	287	SHDR. GRAVEL
37+00	-	53+02	STH 13	RT	71	SHDR. GRAVEL
53+02	-	57+79	STH 13	RT	27	BARRIER SYSTEM
57+79	-	205+50	STH 13	RT	656	SHDR. GRAVEL
205+50	-	211+00	STH 13	RT	29	BARRIER SYSTEM
211+00	-	231+26	STH 13	RT	90	SHDR. GRAVEL
231+26	-	246+24	STH 13	RT	72	BARRIER SYSTEM
246+24	-	355+44	STH 13	RT	485	SHDR. GRAVEL
355+44	-	362+01	STH 13	RT	33	BARRIER SYSTEM
362+01	-	429+00	STH 13	RT	298	SHDR. GRAVEL
429+00		450+06	STH 13	RT	94	BARRIER SYSTEM
450+06		466+66	STH 13	RT	74	SHDR. GRAVEL
466+66	-	475+92	STH 13	RT	18	BARRIER SYSTEM
475+92	-	585+01	STH 13	RT	485	SHDR. GRAVEL
585+01		588+77	STH 13	RT	17	BARRIER SYSTEM
588+77		589+75	STH 13	RT	4	SHDR. GRAVEL
589+75	-	592+16	STH 13	RT	21	BARRIER SYSTEM
592+16	-	597+65	STH 13	RT	24	SHDR. GRAVEL
597+65	-	602+10	STH 13	RT	24	BARRIER SYSTEM
37+50			SUPERIOR AVE.		5	SHDR. GRAVEL
62+04			WILLIAMSON RD.		10	**BOTH
72+03			GARY RD.		10	**BOTH
108+34			HOUGHTON FALLS RD.		10	**BOTH
121+55			MAKI RD.		10	**BOTH
147+88			FROSTMAN RD.		10	**BOTH
174+08			BURLAGER RD.		5	SHDR. GRAVEL
174+09			BODIN RD.		10	**BOTH
198+20			HOWELL RD.		10	**BOTH
210+40			SIOUX RIVER HEIGHTS RD.		30	GRAVEL SURFACE
251+10			FRIENDLY VALLEY RD. WEST		5	SHDR. GRAVEL
251+76			FRIENDLY VALLEY RD. EXT.		10	**BOTH
255+85			KJARVICK RD.		10	**BOTH
290+50			BAYVIEW PARK RD.		10	**BOTH
312+80			MCCULLOCH RD.		10	**BOTH
383+00			WHITING RD.		10	**BOTH
429+20			S. KENNEDY RD.		10	**BOTH
456+68			SKI HILL RD.		5	SHDR. GRAVEL
479+85			HATCHERY RD .		5	SHDR. GRAVEL
497+35			PORT SUPERIOR RD.		5	SHDR. GRAVEL
511+30			PIKES BAY RD.		5	SHDR. GRAVEL
521+80			CHEQUAMEGON RD.		5	SHDR. GRAVEL
527+55			WEBER RD.		5	SHDR. GRAVEL
544+90			OLD SAN RD.		5	SHDR. GRAVEL
566+00			CTH J		10	**BOTH
589+20			LAKESHORE DR.		10	**BOTH
595+55			9TH ST.		5	SHDR. GRAVEL
600+10			PAYNE AVE.		5	SHDR. GRAVEL
			TOTAL		4974	

*SEE ADDITIONAL QUANTITY FOR BAD 3/4-INCH IN 'DRIVEWAYS' MQ TABLE.

** 'BOTH' INDICATES SHOULDER GRAVEL & FEATHER TO MATCH EXISTING GRAVEL SURFACE.

DRIVEWAY AND ASSOCIATED ITEMS

STATION			204.0110 REMOVING ASPHALTIC SURFACE SY	305.0110* BASE AGGREGATE DENSE 3/4-INCH TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON
CAT 0010					
37+10	STH 13'N'	LT	-	10	-
39+25	STH 13'N'	RT	17	-	3
52+20	STH 13'N'	RT	0	10	-
57+80	STH 13'N'	RT	0	10	-
68+10	STH 13'N'	LT	0	10	-
72+80	STH 13'N'	LT	0	10	-
75+95	STH 13'N'	RT	0	10	-
80+75	STH 13'N'	LT	0	10	-
85+15	STH 13'N'	LT	0	10	-
85+75	STH 13'N'	RT	0	10	-
88+35	STH 13'N'	RT	0	10	-
91+25	STH 13'N'	RT	0	10	-
95+55	STH 13'N'	LT	0	10	-
97+45	STH 13'N'	RT	62	-	11
98+50	STH 13'N'	LT	63	-	11
100+75	STH 13'N'	LT	67	-	12
107+30	STH 13'N'	LT	0	10	-
109+65	STH 13'N'	LT	0	10	-
123+30	STH 13'N'	RT	0	10	-
127+25	STH 13'N'	LT	0	10	-
134+85	STH 13'N'	LT	0	10	-
142+35	STH 13'N'	LT	67	-	12
144+25	STH 13'N'	RT	45	-	8
144+80	STH 13'N'	LT	0	10	-
148+85	STH 13'N'	LT	45	-	8
150+60	STH 13'N'	RT	0	10	-
153+25	STH 13'N'	RT	45	-	8
155+00	STH 13'N'	LT	0	10	-
155+15	STH 13'N'	RT	45	-	8
161+25	STH 13'N'	LT	0	10	-
163+75	STH 13'N'	RT	0	10	-
167+30	STH 13'N'	LT	0	10	-
170+50	STH 13'N'	LT	0	10	-
182+15	STH 13'N'	RT	0	10	-
185+85	STH 13'N'	LT	0	10	-
187+25	STH 13'N'	RT	0	10	-
188+20	STH 13'N'	LT	45	-	8
190+90	STH 13'N'	RT	89	-	15
192+45	STH 13'N'	RT	89	-	15
193+00	STH 13'N'	LT	0	10	-
194+80	STH 13'N'	RT	0	10	-
195+75	STH 13'N'	LT	0	10	-
202+00	STH 13'N'	RT	0	10	-
202+05	STH 13'N'	LT	0	10	-
227+00	STH 13'N'	LT	0	10	-
229+50	STH 13'N'	LT	0	10	-
237+60	STH 13'N'	RT	0	20	-
245+25	STH 13'N'	RT	0	10	-
249+10	STH 13'N'	RT	0	10	-
256+35	STH 13'N'	RT	0	10	-
270+15	STH 13'N'	LT	0	10	-
328+20	STH 13'N'	LT	0	10	-
329+30	STH 13'N'	RT	0	10	-
330+00	STH 13'N'	LT	0	10	-
330+75	STH 13'N'	LT	0	10	-
332+90	STH 13'N'	LT	0	10	-
340+30	STH 13'N'	RT	0	10	-
341+30	STH 13'N'	RT	0	10	-
341+60	STH 13'N'	RT	0	10	-
343+35	STH 13'N'	RT	0	10	-
347+75	STH 13'N'	RT	0	10	-
352+45	STH 13'N'	RT	0	10	-
355+50	STH 13'N'	LT	0	10	-
356+40	STH 13'N'	RT	0	10	-
361+00	STH 13'N'	RT	0	10	-
364+40	STH 13'N'	RT	45	-	8
367+00	STH 13'N'	RT	0	10	-
	SUBTOTALS		724	550	127

DRIVEWAY AND ASSOCIATED ITEMS CONT'D

STATION			204.0110 REMOVING ASPHALTIC SURFACE SY	305.0110* BASE AGGREGATE DENSE 3/4-INCH TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON
369+50	STH 13'N'	RT	0	10	-
390+90	STH 13'N'	RT	45	-	8
399+05	STH 13'N'	RT	0	10	-
408+00	STH 13'N'	RT	0	10	-
412+70	STH 13'N'	RT	0	10	-
413+28	STH 13'N'	LT	0	10	-
421+10	STH 13'N'	RT	0	10	-
428+90	STH 13'N'	RT	45	-	8
437+95	STH 13'N'	RT	0	10	-
438+85	STH 13'N'	LT	45	-	8
443+55	STH 13'N'	LT	0	10	-
446+10	STH 13'N'	LT	67	-	12
450+25	STH 13'N'	LT	0	10	-
450+25	STH 13'N'	RT	0	10	-
450+65	STH 13'N'	RT	45	-	8
451+95	STH 13'N'	LT	0	10	-
454+00	STH 13'N'	RT	0	10	-
461+40	STH 13'N'	RT	45	-	8
463+85	STH 13'N'	LT	0	10	-
464+10	STH 13'N'	RT	45	-	8
466+20	STH 13'N'	RT	0	10	-
466+15	STH 13'N'	RT	0	10	-
466+20	STH 13'N'	LT	45	-	8
477+05	STH 13'N'	RT	0	10	-
477+15	STH 13'N'	LT	78	10	14
494+00	STH 13'N'	LT	0	10	-
495+85	STH 13'N'	RT	0	10	-
501+05	STH 13'N'	LT	0	10	-
511+30	STH 13'N'	RT	123	10	21
511+68	STH 13'N'	LT	53	10	9
513+15	STH 13'N'	LT	0	10	-
515+55	STH 13'N'	RT	0	10	-
524+10	STH 13'N'	RT	0	10	-
524+45	STH 13'N'	LT	45	-	8
525+80	STH 13'N'	RT	0	10	-
527+50	STH 13'N'	RT	0	10	-
529+05	STH 13'N'	RT	0	10	-
533+60	STH 13'N'	LT	0	10	-
534+60	STH 13'N'	RT	0	10	-
535+40	STH 13'N'	LT	0	10	-
539+20	STH 13'N'	LT	0	10	-
541+45	STH 13'N'	RT	0	10	-
544+75	STH 13'N'	LT	0	10	-
546+25	STH 13'N'	RT	0	10	-
560+65	STH 13'N'	RT	87	-	15
565+95	STH 13'N'	RT	45	-	8
570+15	STH 13'N'	RT	0	10	-
570+40	STH 13'N'	LT	45		8
570+80	STH 13'N'	RT	0	10	-
577+40	STH 13'N'	RT	45	-	8
578+00	STH 13'N'	RT	45	-	8
584+10	STH 13'N'	LT	0	10	-
584+75	STH 13'N'	RT	45	-	8
585+20	STH 13'N'	LT	0	10	-
589+95	STH 13'N'	RT	45	-	8
590+45	STH 13'N'	LT	0	10	-
592+45	STH 13'N'	RT	0	20	-
593+85	STH 13'N'	LT	0	10	-
597+50	STH 13'N'	RT	45	-	8
600+55	STH 13'N'	LT	45	-	8
601+95	STH 13'N'	LT	89	-	15
602+25	STH 13'N'	RT	0	10	-
UNDISTRIBUTED	STH 13'N'		---	100	9
	SUBTOTALS		1217	540	223
	TOTALS		1941	1090	350

*SEE ADDITIONAL QUANTITY FOR BAD 3/4-INCH IN ITEM MQ TABLE.

HMA PAVEMENT

STATION	-	STATION	LOCATION	WIDTH	DEPTH INCH	460.6224 HMA PAVEMENT 4 MT 58-28 S (LOWER LAYER) TON	460.8444 HMA PAVEMENT 4 SMA 58-34 H (UPPER LAYER) TON	460.6244 HMA PAVEMENT 4 MT 58-34 S (UPPER - SHOULDERS) TON	455.0605 * TACK COAT - GAL	COMMENTS
CAT. 0010										
37+00	-	602+84	STH 13	---	---	22135	15342	6096	22801	MAINLINE AND SHOULDERS
37+50				---	1.75	---	---	17	11	SUPERIOR AVE
62+04				---	1.75	---	---	16	10	WILLIAMSON RD PAVED APRON
72+03				---	1.75	---	---	23	14	GARY RD PAVED APRON
108+34				---	1.75	---	---	23	14	HOUGHTON FALLS RD PAVED APRON
121+55				---	1.75	---	---	18	11	MAKI RD PAVED APRON
147+88				---	1.75	---	---	18	11	FROSTMAN RD PAVED APRON
174+08				---	1.75	---	---	19	12	BURLAGER RD
174+09				---	1.75	---	---	19	12	BODIN RD PAVED APRON
198+20				---	1.75	---	---	21	13	HOWELL RD PAVED APRON
251+10				---	1.75	---	---	23	14	FRIENDLY VALLEY RD (WEST)
251+76				---	1.75	---	---	21	13	FRIENDLY VALLEY RD (EAST) PAVED APRON
255+85				---	1.75	---	---	31	19	KJARVICK RD PAVED APRON
290+50				---	1.75	---	---	19	12	BAYVIEW PARK RD PAVED APRON
312+80				---	1.75	---	---	15	9	MCCULLOCH RD PAVED APRON
383+00				---	1.75	---	---	40	24	WHITING RD PAVED APRON
429+20				---	4.00	---	---	19	0	S. KENNEDY RD - ADD PAVED APRON
456+68				---	1.75	---	---	26	16	SKI HILL RD
479+85				---	1.75	---	---	17	10	HATCHERY RD
497+35				---	1.75	---	---	30	19	PORT SUPERIOR RD
511+30				---	1.75	---	---	19	11	PIKES BAY RD
521+80				---	1.75	---	---	20	12	CHEQUAMEGON RD
527+55				---	1.75	---	---	21	13	WEBER RD
544+90				---	1.75	---	---	52	32	OLD SAN RD
566+00				---	1.75	---	---	21	13	CTH J
589+20				---	1.75	---	---	20	12	LAKESHORE DR PAVED APRON
595+55				---	1.75	---	---	16	10	9TH ST
600+10				---	1.75	---	---	24	15	PAYNE AVE
			TOTALS			22135	15342	6704	23163	

* SEE ADDITIONAL TACK COAT QUANTITY IN ASPHALTIC SURFACE TABLE.

REHEATING JOINTS

STATION	-	STATION	LOCATION	JOINTS	460.4110.S REHEATING HMA PAVEMENT LONGITUDINAL JOINTS LF
CAT. 0010					
37+00	-	232+33	STH 13	1	19533
233+59	-	239+05	STH 13	1	546
240+30	-	320+35	STH 13	1	8005
320+87	-	337+00	STH 13	1	1613
337+00	-	351+00	STH 13	2	2800
351+00	-	455+35	STH 13	1	10435
455+35	-	458+24	STH 13	2	578
458+24	-	486+90	STH 13	1	2866
486+90	-	537+80	STH 13	2	10180
537+80	-	602+84	STH 13	1	6504
			TOTAL		63060

FINISHING ROADWAY

LOCATION	213.0100 FINISHING ROADWAY 01. 8160-03-70 EACH
CAT. 0010	
STH 13	1
PROJECT TOTAL	1

ASPHALTIC SURFACE

STATION	-	STATION	LOCATION	465.0105 ASPHALTIC SURFACE TON	455.0605 * TACK COAT GAL	COMMENT
CAT. 0010						
37+00	-	602+84	STH 13	4000	500	LEVELING & WEDGING
			TOTALS	4000	500	

*SEE ADDITIONAL QUANTITY FOR TACK COAT IN HMA PAVEMENT TABLE.

ASPHALTIC SURFACE PATCHING

STATION	-	STATION	LOCATION	465.0110 ASPHALTIC SURFACE PATCHING TON	COMMENTS
CAT. 0010					
37+00	-	602+84	STH 13	250	MINOR REPAIRS
			TOTAL	250	

ASPHALTIC CURB

STATION	-	STATION	LOCATION	465.0310 ASPHALTIC CURB LF
CAT. 0010				
598+98	-	601+46	STH 13 RT	248
			TOTAL	248

MAINTENANCE OF HAUL ROADS

LOCATION	618.0100 MAINTENANCE AND REPAIR OF HAUL ROADS 01. 8160-03-70 EACH
CAT 0010	
STH 13	1
PROJECT TOTAL	1

MOBILIZATION

LOCATION	619.1000 MOBILIZATION EACH
CAT 0010	
STH 13	1
PROJECT TOTAL	1

FIELD OFFICE TYPE B

LOCATION	642.5001 FIELD OFFICE TYPE B EACH
CAT 0010	
STH 13	1
PROJECT TOTAL	1

3

ASPHALTIC SHOULDER RUMBLE STRIPS

STATION		STATION	LOCATION		465.0425 ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL LF	COMMENTS
CAT. 0010						
37+00	-	61+70	STH 13'N'	LT	2470	TYPE 1
37+80	-	71+50	STH 13'N'	RT	3370	TYPE 1
62+40	-	121+20	STH 13'N'	LT	5880	TYPE 1
72+55	-	107+75	STH 13'N'	RT	3520	TYPE 1
108+75	-	147+30	STH 13'N'	RT	3855	TYPE 1
121+90	-	173+50	STH 13'N'	LT	5160	TYPE 1
148+30	-	173+50	STH 13'N'	RT	2520	TYPE 1
174+50	-	190+40	STH 13'N'	RT	1590	TYPE 1
174+50	-	210+00	STH 13'N'	LT	3550	TYPE 1
192+75	-	197+70	STH 13'N'	LT	495	TYPE 1
198+65	-	232+30	STH 13'N'	RT	3365	TYPE 1
210+90	-	226+75	STH 13'N'	LT	1585	TYPE 1
227+35	-	229+20	STH 13'N'	LT	185	TYPE 1
229+85	-	232+30	STH 13'N'	LT	245	TYPE 1
233+55	-	237+00	STH 13'N'	RT	345	TYPE 1
233+55	-	239+05	STH 13'N'	LT	550	TYPE 1
237+80	-	239+05	STH 13'N'	RT	125	TYPE 1
240+30	-	250+50	STH 13'N'	LT	1020	TYPE 1
240+30	-	251+10	STH 13'N'	RT	1080	TYPE 1
251+70	-	255+25	STH 13'N'	LT	355	TYPE 1
252+20	-	290+20	STH 13'N'	RT	3800	TYPE 1
256+50	-	312+30	STH 13'N'	LT	5580	TYPE 1
291+10	-	320+35	STH 13'N'	RT	2925	TYPE 1
313+30	-	320+35	STH 13'N'	LT	705	TYPE 1
320+85	-	382+40	STH 13'N'	LT	6155	TYPE 1
320+85	-	330+85	STH 13'N'	RT	1000	TYPE 1
371+10	-	454+00	STH 13'N'	RT	8290	TYPE 1
383+85	-	429+00	STH 13'N'	LT	4515	TYPE 1
429+50	-	456+30	STH 13'N'	LT	2680	TYPE 1
457+25	-	468+95	STH 13'N'	LT	1170	TYPE 1
459+60	-	469+60	STH 13'N'	RT	1000	TYPE 1
473+45	-	479+40	STH 13'N'	LT	595	TYPE 1
473+45	-	479+00	STH 13'N'	RT	555	TYPE 1
480+30	-	527+00	STH 13'N'	LT	4670	TYPE 1
528+00	-	560+00	STH 13'N'	LT	3200	TYPE 1
545+25	-	560+00	STH 13'N'	RT	1475	TYPE 1
C.E., P.E. DEDUCTION			STH 13'N'	-	-8250	110 ENTRANCES @ 75'/ENTRANCE
TOTAL					81330	

ASPHALT CENTER LINE RUMBLE STRIPS

STATION		STATION	LOCATION	465.0475 ASPHALT CENTER LINE RUMBLE STRIP 2-LANE RURAL LF	COMMENTS
CAT. 0010					
41+60	-	60+04	STH 13'N'	1844	45 MPH SPEED LIMIT-WILLIAMS RD
64+04	-	67+05	STH 13'N'	301	WILLIAMS RD - C.E. LT
74+03	-	79+75	STH 13'N'	572	GARY RD - C.E. LT
81+75	-	84+20	STH 13'N'	245	C.E. LT - C.E. LT
89+35	-	99+75	STH 13'N'	1040	C.E. RT - C.E. LT
101+75	-	106+30	STH 13'N'	455	C.E. LT - HOUGHTON FALLS RD
110+65	-	119+55	STH 13'N'	890	HOUGHTON FALLS RD - MAKI RD
123+55	-	126+25	STH 13'N'	270	MAKI RD - C.E. LT
128+25	-	145+88	STH 13'N'	1763	C.E. LT - FROSTMAN RD
149+88	-	172+08	STH 13'N'	2220	FROSTMAN RD - BURLAGER RD
176+10	-	196+20	STH 13'N'	2010	BODIN RD - HOWELL DR
203+05	-	226+00	STH 13'N'	2295	C.E. LT - WAYSIDE LT
233+99	-	238+64	STH 13'N'	465	SIOUX R BRIDGE - SIOUX R BRIDGE
240+71	-	249+10	STH 13'N'	839	SIOUX R BR - FRIENDLY VALLEY RD
257+85	-	288+50	STH 13'N'	3065	KJARVICK RD - BAYVIEW PARK RD
292+50	-	310+80	STH 13'N'	1830	BAYVIEW PARK RD-MCCULLOCH RD
314+80	-	319+94	STH 13'N'	514	MCCULLOCH RD - ONION R BR
321+28	-	329+00	STH 13'N'	772	ONION R BR - C.E. LT
331+00	-	381+00	STH 13'N'	5000	C.E. LT - WHITING RD
335+00	-	445+10	STH 13'N'	11010	WHITING RD - C.E. LT
447+10	-	454+68	STH 13'N'	758	C.E. LT - SKI HILL RD
458+68	-	460+40	STH 13'N'	172	SKI HILL RD - C.E. RT
465+10	-	470+47	STH 13'N'	537	C.E. RT - PIKES CREEK BR
472+56	-	476+15	STH 13'N'	359	PIKES CREEK BRIDGE - C.E. LT
481+85	-	495+35	STH 13'N'	1350	HATCHERY RD-PORT SUPERIOR RD
499+35	-	510+30	STH 13'N'	1095	PORT SUPERIOR RD - C.E. RT
512+30	-	519+80	STH 13'N'	750	C.E. RT - CHEQUAMEGON RD
529+55	-	532+60	STH 13'N'	305	WEBER RD - C.E. LT
536+40	-	540+45	STH 13'N'	405	C.E. LT - C.E. RT
546+90	-	559+60	STH 13'N'	1270	OLD SAN RD-45 MPH SPEED LIMIT
TOTAL				44401	

SILT FENCE ITEMS

STATION		STATION	LOCATION		628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF
CAT. 0010						
54+50	-	57+70	STH 13 'N'	RT	331	331
205+60	-	208+30	STH 13 'N'	RT	270	270
208+55	-	210+85	STH 13 'N'	RT	230	230
205+65	-	208+28	STH 13 'N'	LT	271	271
208+55	-	209+80	STH 13 'N'	LT	125	125
229+43	-	232+45	STH 13 'N'	LT	302	302
233+52	-	239+15	STH 13 'N'	LT	573	573
240+25	-	250+35	STH 13 'N'	LT	1020	1020
231+12	-	232+48	STH 13 'N'	RT	145	145
233+52	-	239+15	STH 13 'N'	RT	575	575
240+25	-	243+00	STH 13 'N'	RT	275	275
244+35	-	246+34	STH 13 'N'	RT	199	199
359+25	-	361+00	STH 13 'N'	RT	175	175
428+85	-	429+10	STH 13 'N'	LT	40	40
429+30	-	429+50	STH 13 'N'	LT	35	35
428+90	-	430+50	STH 13 'N'	RT	162	162
444+75	-	446+40	STH 13 'N'	RT	172	172
464+10	-	470+75	STH 13 'N'	LT	665	665
472+10	-	476+78	STH 13 'N'	LT	476	476
466+95	-	470+90	STH 13 'N'	RT	395	395
472+32	-	475+70	STH 13 'N'	RT	350	350
591+12	-	592+42	STH 13 'N'	RT	136	136
597+65		598+90	STH 13 'N'	RT	135	135
600+70		601+80	STH 13 'N'	RT	95	95
UNDISTRIBUTED					750	750
TOTALS					7902	7902

GRADING SHAPING AND FINISHING INTERSECTION 01. S. KENNEDY RD

STATION	LOCATION		205.9015.S GRADING SHAPING AND FINISHING INTERSECTION LS	EXCAVATION COMMON* CY	BORROW* CY	TOPSOIL* SY	SEEDING MIXTURE NO. 10* LB	SEEDING TEMPORARY* LB	FERTILIZER TYPE B* CWT	MULCH * SY
CAT. 0010										
429+20	STH 13'N'	LT	1	37	0	34	1	1	0.0	34
TOTALS			1	37	0	34	1	1	0.0	34

* ITEMS AND QUANTITIES LISTED FOR BID INFORMATION ONLY

EROSION MAT

STATION		STATION	LOCATION		628.2008 EROSION MAT URBAN CLASS I TYPE B SY
CAT. 0010					
53+85	-	54+75	STH 13	RT	163
56+50	-	57+55	STH 13	RT	192
205+75	-	206+40	STH 13	RT	110
205+83	-	208+25	STH 13	LT	689
229+55	-	232+25	STH 13	LT	242
233+70	-	239+00	STH 13	LT	448
234+58	-	235+60	STH 13	RT	147
240+45	-	250+00	STH 13	LT	1413
241+55	-	245+00	STH 13	RT	305
359+50	-	361+00	STH 13	RT	278
429+00	-	430+25	STH 13	RT	264
445+00	-	446+25	STH 13	RT	164
464+50	-	470+65	STH 13	LT	625
473+07	-	475+10	STH 13	LT	176
474+45	-	475+45	STH 13	RT	94
591+46	-	592+16	STH 13	RT	77
597+80	-	598+50	STH 13	RT	119
600+90	-	601+80	STH 13	RT	116
UNDISTRIBUTED					725
TOTAL					6347

MOBILIZATIONS EROSION CONTROL

LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
CAT 0010		
STH 13	2	2
PROJECT TOTALS	2	2

GUARDRAIL COMPONENTS SUMMARY

STATION	-	STATION	LOCATION	614.2300 MGS GUARDRAIL 3 LF	614.2330 MGS GUARDRAIL 3K LF	614.2340 MGS GUARDRAIL 3L LF	614.2500 MGS THRIE BEAM TRANSITION LF	614.2610 MGS GUARDRAIL EAT TERMINAL EACH	614.0305 STEEL PLATE BEAM GUARD CLASS A LF	614.0345 STEEL PLATE BEAM GUARD SHORT RADIUS LF	614.0370 STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL EACH	614.0390 STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL EACH
CAT. 0010												
54+20.7	-	54+73.8	STH 13'N' RT	-	-	-	-	1	-	-	-	-
54+73.8	-	56+98.8	STH 13'N' RT	225	-	-	-	-	-	-	-	-
56+98.8	-	57+51.9	STH 13'N' RT	-	-	-	-	1	-	-	-	-
205+83.8	-	206+36.9	STH 13'N' RT	-	-	-	-	1	-	-	-	-
206+36.9	-	207+86.9	STH 13'N' RT	150	-	-	-	-	-	-	-	-
207+86.9	-	208+99.4	STH 13'N' RT	-	-	112.5	-	-	-	-	-	-
208+99.4	-	209+49.4	STH 13'N' RT	50	-	-	-	-	-	-	-	-
209+49.4	-	210+02.5	STH 13'N' RT	-	-	-	-	1	-	-	-	-
206+96.3	-	207+49.4	STH 13'N' LT	-	-	-	-	1	-	-	-	-
207+49.4	-	207+86.9	STH 13'N' LT	37.5	-	-	-	-	-	-	-	-
207+86.9	-	208+99.4	STH 13'N' LT	-	-	112.5	-	-	-	-	-	-
208+99.4	-	209+36.9	STH 13'N' LT	37.5	-	-	-	-	-	-	-	-
209+36.9	-	209+90.0	STH 13'N' LT	-	-	-	-	1	-	-	-	-
229+81.6	-	230+34.7	STH 13'N' LT	-	-	-	-	1	-	-	-	-
230+34.7	-	231+84.7	STH 13'N' LT	150	-	-	-	-	-	-	-	-
231+84.7	-	232+24.1	STH 13'N' LT	-	-	-	39.4	-	-	-	-	-
231+26.0	-	231+88.5	STH 13'N' RT	62.5	-	-	-	-	-	-	-	-
231+88.5	-	232+27.9	STH 13'N' RT	-	-	-	39.4	-	-	-	-	-
233+66.3	-	234+05.7	STH 13'N' RT	-	-	-	39.4	-	-	-	-	-
234+05.7	-	234+30.7	STH 13'N' RT	25	-	-	-	-	-	-	-	-
234+30.7	-	234+83.8	STH 13'N' RT	-	-	-	-	1	-	-	-	-
233+72.3	-	234+11.7	STH 13'N' LT	-	-	-	39.4	-	-	-	-	-
234+11.7	-	234+24.2	STH 13'N' LT	12.5	-	-	-	-	-	-	-	-
234+24.2	-	238+46.8	STH 13'N' LT	-	425	-	-	-	-	-	-	-
238+46.8	-	238+59.3	STH 13'N' LT	12.5	-	-	-	-	-	-	-	-
238+59.3	-	238+98.7	STH 13'N' LT	-	-	-	39.4	-	-	-	-	-
237+82.5	-	238+35.6	STH 13'N' RT	-	-	-	-	1	-	-	-	-
238+35.6	-	238+60.6	STH 13'N' RT	25	-	-	-	-	-	-	-	-
238+60.6	-	239+00.0	STH 13'N' RT	-	-	-	39.4	-	-	-	-	-
240+37.6	-	240+77.0	STH 13'N' RT	-	-	-	39.4	-	-	-	-	-
240+77.0	-	244+52.0	STH 13'N' RT	375	-	-	-	-	-	-	-	-
244+52.0	-	245+05.1	STH 13'N' RT	-	-	-	-	1	-	-	-	-
240+49.8	-	240+89.2	STH 13'N' LT	-	-	-	39.4	-	-	-	-	-
240+89.2	-	247+64.2	STH 13'N' LT	675	-	-	-	-	-	-	-	-
247+64.2	-	248+17.3	STH 13'N' LT	-	-	-	-	1	-	-	-	-
356+63.7	-	357+16.8	STH 13'N' RT	-	-	-	-	1	-	-	-	-
357+16.8	-	357+41.8	STH 13'N' RT	25	-	-	-	-	-	-	-	-
357+41.8	-	359+29.3	STH 13'N' RT	-	187.5	-	-	-	-	-	-	-
359+29.3	-	360+29.3	STH 13'N' RT	100	-	-	-	-	-	-	-	-
360+29.3	-	360+82.4	STH 13'N' RT	-	-	-	-	1	-	-	-	-
429+75.2	-	430+25.2	STH 13'N' RT	-	-	-	-	-	-	-	1	-
430+25.2	-	437+56.5	STH 13'N' RT	-	-	-	-	-	731.25	-	-	-
437+56.5	-	437+80.0	STH 13'N' RT	-	-	-	-	-	-	43.75	-	-
437+80.0	-	437+78.2	STH 13'N' RT	-	-	-	-	-	-	-	-	1
438+01.2	-	438+04.9	STH 13'N' RT	-	-	-	-	-	-	-	-	1
438+04.9	-	438+33.6	STH 13'N' RT	-	-	-	-	-	-	43.75	-	-
438+33.6	-	449+83.6	STH 13'N' RT	-	-	-	-	-	1150	-	-	-
449+83.6	-	450+11.0	STH 13'N' RT	-	-	-	-	-	-	43.75	-	-
450+11.0	-	450+05.8	STH 13'N' RT	-	-	-	-	-	-	-	-	1
467+95.2	-	468+48.3	STH 13'N' RT	-	-	-	-	1	-	-	-	-
468+48.3	-	470+48.3	STH 13'N' RT	200	-	-	-	-	-	-	-	-
470+48.3	-	470+87.7	STH 13'N' RT	-	-	-	39.4	-	-	-	-	-
465+94.9	-	466+48.0	STH 13'N' LT	-	-	-	-	1	-	-	-	-
466+48.0	-	468+48.0	STH 13'N' LT	200	-	-	-	-	-	-	-	-
468+48.0	-	469+48.0	STH 13'N' LT	-	100	-	-	-	-	-	-	-
469+48.0	-	470+23.0	STH 13'N' LT	75	-	-	-	-	-	-	-	-
470+23.0	-	470+62.4	STH 13'N' LT	-	-	-	39.4	-	-	-	-	-
472+16.9	-	472+56.3	STH 13'N' LT	-	-	-	39.4	-	-	-	-	-
472+56.3	-	473+43.8	STH 13'N' LT	87.5	-	-	-	-	-	-	-	-
473+43.8	-	474+43.8	STH 13'N' LT	-	-	100	-	-	-	-	-	-
474+43.8	-	475+06.3	STH 13'N' LT	62.5	-	-	-	-	-	-	-	-
475+06.3	-	475+59.4	STH 13'N' LT	-	-	-	-	1	-	-	-	-
SUBTOTALS				2587.5	712.5	325	433.4	16	1881.25	131.25	1	3

GUARDRAIL COMPONENTS SUMMARY CONT'D

STATION	-	STATION	LOCATION	614.2300 MGS GUARDRAIL 3 LF	614.2330 MGS GUARDRAIL 3K LF	614.2340 MGS GUARDRAIL 3L LF	614.2500 MGS THRIE BEAM TRANSITION LF	614.2610 MGS GUARDRAIL EAT EACH	614.0305 STEEL PLATE BEAM GUARD CLASS A LF	614.0345 STEEL PLATE BEAM GUARD SHORT RADIUS LF	614.0370 STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL EACH	614.0390 STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL EACH
CAT. 0010												
472+42.7	-	472+82.1	STH 13'N' RT	-	-	-	39.4	-	-	-	-	-
472+82.1	-	474+19.6	STH 13'N' RT	137.5	-	-	-	-	-	-	-	-
474+19.6	-	474+72.7	STH 13'N' RT	-	-	-	-	1	-	-	-	-
584+86.0	-	584+93.3	STH 13'N' RT	-	-	-	-	-	-	-	-	1
584+93.3	-	585+28.3	STH 13'N' RT	-	-	-	-	-	-	43.75	-	-
585+28.3	-	586+72.1	STH 13'N' RT	-	-	-	-	-	143.75	-	-	-
586+72.1	-	587+22.1	STH 13'N' RT	-	-	-	-	-	-	-	1	-
590+20.1	-	590+73.2	STH 13'N' RT	-	-	-	-	1	-	-	-	-
590+73.2	-	591+48.2	STH 13'N' RT	75	-	-	-	-	-	-	-	-
591+48.2	-	592+01.3	STH 13'N' RT	-	-	-	-	1	-	-	-	-
597+80.1	-	598+33.2	STH 13'N' RT	-	-	-	-	1	-	-	-	-
598+33.2	-	601+43.5	STH 13'N' RT	275	-	-	-	-	-	-	-	-
601+43.5	-	601+96.6	STH 13'N' RT	-	-	-	-	1	-	-	-	-
		SUBTOTALS		487.5	0	0	39.4	5	143.75	43.75	1	1
			TOTALS	3075	712.5	325	472.8	21	2025	175	2	4

NOTE: SEGMENT OF MGS GUARDRAIL 3K WILL REQUIRE CUTTING TO REMOVE 2.4' IN SPAN FROM STA 234+24.2 TO STA 238+46.8 LT

BARRIER SYSTEM GRADING SHAPING FINISHING

STATION	-	STATION	LOCATION	614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING EACH	EXCAVATION COMMON* CY	FILL X 1.25* CY	BORROW* CY	TOPSOIL* SY	SEEDING MIXTURE NO. 10* LB	SEEDING TEMPORARY* LB	FERTILIZER TYPE B* CWT	MULCH * SY
CAT. 0010												
53+02	-	57+79	STH 13'N' RT	1	20	106	86	698	13	26	0.6	948
205+83	-	210+00	STH 13'N' LT	1	489	55	0	684	12	25	0.6	906
205+50	-	211+00	STH 13'N' RT	1	46	38	0	405	9	17	0.4	627
228+40	-	250+00	STH 13'N' LT	1	16	1243	1227	2098	42	85	2.0	1073
231+26	-	246+24	STH 13'N' RT	1	54	147	93	575	17	34	0.8	838
355+44	-	362+01	STH 13'N' RT	1	15	158	143	647	14	28	0.6	756
429+00	-	450+06	STH 13'N' RT	1	10	17	7	521	23	46	1.1	1691
464+10	-	476+78	STH 13'N' LT	1	4	603	599	1264	24	48	1.1	1252
466+66	-	475+92	STH 13'N' RT	1	14	92	78	295	10	20	0.5	467
585+01	-	588+77	STH 13'N' RT	1	10	17	7	280	7	14	0.3	489
589+75	-	592+16	STH 13'N' RT	1	15	17	2	97	3	7	0.2	154
597+65	-	602+10	STH 13'N' RT	1	13	105	92	270	7	14	0.3	291
			TOTALS	12	706	2598	2334	7834	180	364	8.4	9492

* ITEMS AND QUANTITIES LISTED FOR BID INFORMATION ONLY

REMOVING GUARDRAIL

STATION	-	STATION	LOCATION	204.0165 REMOVING GUARDRAIL LF
CAT. 0010				
230+05	-	232+24	STH 13 LT	219
231+26	-	232+28	STH 13 RT	102
233+66	-	234+64	STH 13 RT	98
233+72	-	234+77	STH 13 LT	105
237+90	-	239+99	STH 13 LT	209
237+91	-	239+00	STH 13 RT	109
240+37	-	241+48	STH 13 RT	111
240+50	-	241+60	STH 13 LT	110
356+80	-	359+09	STH 13 RT	229
468+33	-	470+62	STH 13 LT	229
469+63	-	470+88	STH 13 RT	125
472+17	-	474+17	STH 13 LT	200
472+43	-	473+66	STH 13 RT	123
598+99	-	601+98	STH 13 RT	299
			TOTAL	2268

PERMANENT SIGNING ITEMS

STH 13 STATION (APPROX.)		SIGN NO.	SIGN CODE	SIGN MESSAGE	SIGN SIZE W X H (INCHES)	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	COMMENT
CAT. 0010												
37+78	RT	1-1	R1-1	STOP	30 X 30	5.18	---	---	1	1	1	SUPERIOR AVE.
40+04	LT	1-2	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
41+59	LT	1-3	R2-1	SPEED LIMIT 45	24 X 30	5.00	---	1	---	1	1	
41+60	RT	1-4	R2-1	SPEED LIMIT 55	24 X 30	5.00	---	1	---	1	1	
43+35	RT	1-5	W1-2L	Left Curve symbol	30 X 30	---	6.25	---	1	1	1	
44+10	LT	1-6	[illegible]	Washburn POPULATION 2117	66 X 24	11.00	---	2	---	1	2	SEE SIGN DETAIL
45+15	RT	1-7	---	NORTH 13	---	---	---	---	---	1	---	REMOVE SIGNS FROM D2-1 POSTS
45+15	RT	1-8	D2-1	Bayfield 12	72 X 15	7.50	---	---	2	1	2	SEE SIGN DETAIL
47+35	RT	1-9	J4-1	NORTH 13	24 X 36	6.00	---	---	1	---	---	
49+58	RT	1-10	S3-1	School Bus Stop Ahead symbol	36 X 36	---	9.00	---	1	1	1	
49+62	LT	1-11	W3-5	Reduced Speed Ahead 45	36 X 36	---	9.00	---	1	1	1	
61+58	LT	1-12	R1-1	STOP	30 X 30	5.18	---	---	1	1	1	WILLIAMSON RD.
65+10	RT	1-13	D1-1	Gary Rd (right arrow)	66 X 15	6.88	---	---	2	1	2	SEE SIGN DETAIL
66+06	LT	1-14	W1-2R	Right Curve symbol	30 X 30	---	6.25	---	1	1	1	
68+82	LT	1-15	---	Right Curve symbol	---	---	---	---	---	1	1	
71+44	LT	1-16	I55-56	WASHBURN H.S. ECOLOGY CLUB	30 X 36	7.50	---	---	1	1	1	ADOPT-A-HIGHWAY
72+33	RT	1-17	R1-1	STOP	30 X 30	5.18	---	1	---	1	1	GARY RD.
73+27	RT	1-18	I55-56	WASHBURN BOY SCOUT TROOP 344	30 X 36	7.50	---	---	1	1	1	ADOPT-A-HIGHWAY
78+83	LT	1-19	D1-1	Gary Rd (Left arrow)	66 X 15	6.88	---	---	2	1	2	SEE SIGN DETAIL
96+50	RT	2-1	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	SIGN FACES SOUTHBOUND TRAFFIC
101+33	RT	2-2	D1-1	Houghton Falls Rd (Right arrow)	114 X 15	11.88	---	---	2	1	2	SEE SIGN DETAIL
108+58	RT	2-3	R1-1	STOP	30 X 30	5.18	---	1	---	1	1	HOUGHTON FALLS RD
113+56	LT	2-4	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
114+52	RT	2-5	D1-1	Maki Rd (Left arrow)	60 X 15	6.25	---	---	2	1	2	SEE SIGN DETAIL
115+32	LT	2-6	D1-1	Houghton Falls Rd (Left arrow)	114 X 15	11.88	---	---	2	1	2	SEE SIGN DETAIL
121+28	LT	2-7	R1-1	STOP	30 X 30	5.18	---	---	1	1	1	MAKI RD.
128+46	LT	2-8	D1-1	Maki Rd (Right arrow)	60 X 15	6.25	---	---	2	1	2	SEE SIGN DETAIL
140+89	RT	2-9	D1-1	Frostman Rd (Right arrow)	84 X 15	8.75	---	---	2	1	2	SEE SIGN DETAIL
143+54	RT	2-10	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	SIGN FACES SOUTHBOUND TRAFFIC
148+14	RT	2-11	R1-1	STOP	30 X 30	5.18	---	1	---	1	1	FROSTMAN RD.
149+22	RT	2-12	---	Deer Crossing symbol	---	---	---	---	---	1	1	
155+58	LT	3-1	D1-1	Frostman Rd (Left arrow)	84 X 15	8.75	---	---	2	1	2	SEE SIGN DETAIL
160+10	RT	3-2	D5-63	HISTORICAL MARKER 1/2 MILE	60 X 36	15.00	---	---	2	1	2	
161+70	LT	3-3	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
166+79	RT	3-4	D1-2	Burlager Rd (Left arrow) Bodin Rd (Right arrow)	84 X 30	17.50	---	---	2	1	2	SEE SIGN DETAIL
172+40	LT	3-5	I55-56	WASHBURN BOY SCOUT TROOP 344	30 X 36	7.50	---	---	1	1	1	ADOPT-A-HIGHWAY
173+83	LT	3-6	R1-1	STOP	30 X 30	5.18	---	---	1	1	1	BURLAGER RD.
174+34	RT	3-7	R1-1	STOP	30 X 30	5.18	---	1	---	1	1	BODIN RD.
174+82	LT	3-8	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
175+20	RT	3-9	I55-56	VICA WASHBURN H.S. CHAPTER	30 X 36	7.50	---	---	1	1	1	ADOPT-A-HIGHWAY
178+47	RT	3-10	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	SIGN FACES SOUTHBOUND TRAFFIC
181+08	LT	3-11	D1-2	Bodin Rd (Left arrow) Burlager Rd (Right arrow)	84 X 30	17.50	---	---	2	1	2	SEE SIGN DETAIL
187+00	LT	3-12	---	Deer Crossing symbol	---	---	---	---	---	1	1	
189+25	RT	3-13	W1-2L	Left Curve symbol	36 X 36	---	9.00	---	1	1	1	
191+19	RT	3-15	R1-1	STOP	30 X 30	5.18	---	1	---	1	1	HISTORICAL MARKER ENTRANCE
191+47	RT	3-16	D5-64-R	HISTORICAL MARKER (Right arrow)	60 X 36	15.00	---	---	2	1	2	
192+71	RT	3-17	R1-1	STOP	30 X 30	5.18	---	1	---	1	1	HISTORICAL MARKER ENTRANCE
193+23	RT	3-18	D1-1	Howell Rd (Right arrow)	72 X 15	7.50	---	---	2	1	2	SEE SIGN DETAIL
193+47	LT	3-19	D5-64-L	HISTORICAL MARKER (Left arrow)	60 X 36	15.00	---	---	2	1	2	
198+38	RT	3-20	R1-1	STOP	30 X 30	5.18	---	1	---	1	1	HOWELL RD.
199+12	RT	3-21	S3-1	School Bus Stop Ahead symbol	36 X 36	---	9.00	---	1	1	1	
204+34	LT	3-22	W1-2R	Right Curve symbol	36 X 36	---	9.00	---	1	---	---	NEW LOCATION
206+52	LT	3-24	D1-1	Howell Rd (Left arrow)	72 X 15	7.50	---	---	2	1	2	SEE SIGN DETAIL
208+36	LT	3-25	---	Right Curve symbol w/50 M.P.H.	---	---	---	---	---	1	1	
208+90	RT	3-26	---	Right Curve symbol	---	---	---	---	---	1	1	
209+65	LT	3-27	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	
212+76	RT	3-28	W14-3	NO PASSING ZONE	48 X 36	---	6.00	---	1	1	1	
				SUBTOTALS		284.00	111.50	11	57	55	71	

PERMANENT SIGNING ITEMS CONT'D

STH 13 STATION (APPROX.)		SIGN NO.	SIGN CODE	SIGN MESSAGE	SIGN SIZE W X H (INCHES)	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	COMMENT
218+30	LT	4-1	D5-63	HISTORICAL MARKER 1/2 MILE	60 X 36	15.00	—	—	2	1	2	
227+84	RT	4-2	W14-3	NO PASSING ZONE	48 X 36	—	6.00	—	1	1	1	SIGN FACES SOUTHBOUND TRAFFIC
243+93	RT	4-3	D1-61	Friendly Valley Rd (Left arrow & Right arrow)	102 X 24	17.00	—	—	2	1	2	SEE SIGN DETAIL
246+54	RT	4-4	J4-1	NORTH 13	24 X 36	6.00	—	—	1	—	—	
246+56	LT	4-5	J4-1	SOUTH 13	24 X 36	6.00	—	—	1	—	—	
250+72	LT	4-6	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	FRIENDLY VALLEY RD.
251+99	RT	4-7	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	FRIENDLY VALLEY RD.
252+63	RT	4-8	D1-1	Kjarvick Rd (Left arrow)	84 X 15	8.75	—	—	2	1	2	SEE SIGN DETAIL
252+73	LT	4-9	I55-56	VICA WASHBURN H.S. CHAPTER	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
255+00	LT	4-10	D1-61	Friendly Valley Rd (Left arrow & Right arrow)	102 X 24	17.00	—	—	2	1	2	SEE SIGN DETAIL
255+05	RT	4-11	I55-56	SPINDRIFT CREW	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
255+63	LT	4-12	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	KJARVICK RD.
263+00	LT	4-13	D1-1	Kjarvick Rd (Right arrow)	84 X 15	8.75	—	—	2	1	2	SEE SIGN DETAIL
283+62	RT	5-1	D1-1	Bayview Park Rd (Right arrow)	108 X 15	11.25	—	—	2	1	2	SEE SIGN DETAIL
291+00	RT	5-2	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	BAYVIEW PARK RD.
297+62	LT	5-3	D1-1	Bayview Park Rd (Left arrow)	108 X 15	11.25	—	—	2	1	2	SEE SIGN DETAIL
304+35	RT	5-4	D1-1	McCulloch Rd (Left arrow)	90 X 15	9.38	—	—	2	1	2	SEE SIGN DETAIL
306+35	RT	5-5	R4-51	PASSING LANE AHEAD 1/2 MILE	36 X 48	12.00	—	—	1	—	—	
307+46	LT	5-6	W14-3	NO PASSING ZONE	48 X 36	—	6.00	—	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
311+00	RT	5-7	I55-56	APOSTLE ISLANDS REALTY	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
311+00	LT	5-8	I55-56	SPINDRIFT CREW	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
312+83	LT	5-9	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	MCCULLOCH RD.
313+50	RT	5-10	W1-2L	Right Curve symbol	30 X 30	—	6.25	—	1	1	1	
317+00	RT	5-11	S3-1	School Bus Stop Ahead symbol	36 X 36	—	9.00	—	1	1	1	
319+10	LT	5-12	D1-1	McCulloch Rd (Right arrow)	90 X 15	9.38	—	—	2	1	2	SEE SIGN DETAIL
320+31	RT	5-13	W5-52R	Clearance Striper Down Left	12 X 36	—	3.00	1	—	1	1	
320+32	LT	5-14	W5-52L	Clearance Striper Down Right	12 X 36	—	3.00	1	—	1	1	
320+91	LT	5-15	W5-52R	Clearance Striper Down Left	12 X 36	—	3.00	1	—	1	1	
320+92	RT	5-16	W5-52L	Clearance Striper Down Right	12 X 36	—	3.00	1	—	1	1	
332+60	RT	5-17	R4-3	SLOWER TRAFFIC KEEP RIGHT	36 X 48	12.00	—	1	—	1	1	
338+50	LT	6-1	W1-2L	Left Curve symbol	30 X 30	—	6.25	—	1	—	—	
341+05	RT	6-2	W9-1R	RIGHT LANE ENDS	36 X 36	—	9.00	—	1	1	1	
341+60	LT	6-3	—	Left Curve symbol	—	—	—	—	—	1	1	
345+80	RT	6-4	W4-2R	Right Lane Reduction Transition symbol	36 X 36	—	9.00	—	1	1	1	
345+80	LT	6-5	W4-2R	Right Lane Reduction Transition symbol	36 X 36	—	9.00	—	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
355+40	RT	6-6	—	Left Curve symbol w/50 M.P.H.	—	—	—	—	—	1	1	
355+82	LT	6-7	R3-72	ONCOMING TRAFFIC USES CENTER LANE	30 X 36	7.50	—	1	—	—	—	
361+44	RT	6-8	W1-2R	Left Curve symbol	30 X 30	—	6.25	—	1	—	—	NEW LOCATION
371+21	LT	6-10	W1-2L	Right Curve symbol	30 X 30	—	6.25	—	1	—	—	
375+07	LT	6-11	—	Right Curve symbol	—	—	—	—	—	1	1	
376+78	RT	6-12	D1-1	Whiting Rd (Left arrow)	78 X 15	8.13	—	—	2	1	2	SEE SIGN DETAIL
376+98	RT	6-13	W14-3	NO PASSING ZONE	48 X 36	—	6.00	—	1	1	1	SIGN FACES SOUTHBOUND TRAFFIC
382+65	LT	6-14	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	WHITING RD.
385+24	LT	6-15	W14-3	NO PASSING ZONE	48 X 36	—	6.00	—	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
387+40	RT	6-16	—	Left Curve symbol	—	—	—	—	—	1	1	
390+32	LT	6-17	D1-1	Whiting Rd (Right arrow)	78 X 15	8.13	—	—	2	1	2	SEE SIGN DETAIL
391+40	RT	6-18	W1-2L	Left Curve symbol	30 X 30	—	6.25	—	1	—	—	
401+66	LT	7-1	W1-2L	Right Curve symbol	30 X 30	—	6.25	—	1	1	1	
402+18	RT	7-2	W1-2L	Left Curve symbol	30 X 30	—	6.25	—	1	1	1	
405+18	RT	7-3	S3-1	School Bus Stop Ahead symbol	36 X 36	—	9.00	—	1	1	1	
421+55	LT	7-4	S3-1	School Bus Stop Ahead symbol	36 X 36	—	9.00	—	1	1	1	
427+20	RT	7-5	R4-51	PASSING LANE AHEAD 1 MILE	36 X 48	12.00	—	—	1	—	—	
428+60	LT	7-6	I55-56	APOSTLE ISLANDS REALTY	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
429+00	LT	7-7	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	S. KENNEDY RD.
429+48	RT	7-8	I55-56	BLUE VISTA FARM	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
445+80	RT	7-9	W1-2R	Right Curve symbol	30 X 30	—	6.25	—	1	1	1	
445+80	RT	7-10	W13-1	50 M.P.H.	18 X 18	—	2.25	—	—	—	—	ON SAME POST AS SIGN NO. 7-9
448+47	LT	7-11	W1-2L	Right Curve symbol	30 X 30	—	6.25	—	1	1	1	
449+08	RT	7-12	D1-1	Ski Hill Rd (Left arrow)	78 X 15	8.13	—	—	2	1	2	SEE SIGN DETAIL
451+34	RT	7-13	—	Bayfield Fish Hatchery 1/2 MILE	66 X 36	16.50	—	—	2	1	2	SEE SIGN DETAIL
453+60	RT	7-14	R4-51	PASSING LANE AHEAD 1/2 MILE	36 X 48	12.00	—	—	1	—	—	
				SUBTOTALS		297.41	148.50	6	63	50	63	

PERMANENT SIGNING ITEMS CONT'D

STH 13 STATION (APPROX.)		SIGN NO.	SIGN CODE	SIGN MESSAGE	SIGN SIZE W X H (INCHES)	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	COMMENT
456+48	LT	8-1	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	SKI HILL RD.
456+62	RT	8-2	—	13	—	—	—	—	—	1	—	ON SAME POST AS SIGN NO. 8-3
456+62	RT	8-3	W1-7	Night Arrow (Double)	48 X 24	—	8.00	—	1	—	1	SIGN FACES SKI HILL ROAD TRAFFIC
462+86	RT	8-4	W5-2	NARROW BRIDGE	36 X 36	—	9.00	—	1	1	1	
463+20	LT	8-5	D1-1	Ski Hill Rd (Right arrow)	78 X 15	8.13	—	—	2	1	2	SEE SIGN DETAIL
467+60	RT	8-6	R7-1-L	NO PARKING ANY TIME (Left Arrow)	18 X 24	3.00	—	1	—	1	1	
467+63	LT	8-7	R7-1-R	NO PARKING ANY TIME (Right Arrow)	18 X 24	3.00	—	1	—	1	1	
469+55	RT	8-8	R7-1-D	NO PARKING ANY TIME (Double Arrow)	18 X 24	3.00	—	1	—	1	1	
469+58	LT	8-9	R7-1-D	NO PARKING ANY TIME (Double Arrow)	18 X 24	3.00	—	1	—	1	1	
470+57	LT	8-10	W5-52L	Clearance Striper Down Right	12 X 36	—	3.00	1	—	1	1	
470+82	RT	8-11	W5-52R	Clearance Striper Down Left	12 X 36	—	3.00	1	—	1	1	
470+84	RT	8-12	I-3-1	Pikes Creek	66 X 15	6.88		2	—	1	1	SEE SIGN DETAIL
472+21	LT	8-13	I-3-1	Pikes Creek	66 X 15	6.88		2	—	1	1	SEE SIGN DETAIL
472+23	LT	8-14	W5-52R	Clearance Striper Down Left	12 X 36	—	3.00	1	—	1	1	
472+49	RT	8-15	W5-52L	Clearance Striper Down Right	12 X 36	—	3.00	1	—	1	1	
473+19	RT	8-16	D1-1	Hatchery Rd (Left arrow)	84 X 15	8.75	—	—	2	1	2	SEE SIGN DETAIL
474+00	RT	8-17	R7-1-D	NO PARKING ANY TIME (Double Arrow)	18 X 24	3.00	—	1	—	1	1	
474+00	LT	8-18	R7-1-D	NO PARKING ANY TIME (Double Arrow)	18 X 24	3.00	—	—	1	1	1	
474+00	LT	8-19	W1-2L	Left Curve symbol	30 X 30	—	6.25	—	—	—	—	ON SAME POST & ABOVE SIGN NO. 8-18
475+85	RT	8-20	—	Bayfield Fish Hatchery (Left arrow)	66 X 36	16.50	—	—	2	1	2	SEE SIGN DETAIL
476+02	LT	8-21	R7-1-D	NO PARKING ANY TIME (Double Arrow)	18 X 24	3.00	—	1	—	1	1	
476+15	RT	8-22	R7-1-D	NO PARKING ANY TIME (Double Arrow)	18 X 24	3.00	—	1	—	1	1	
478+09	RT	8-23	R7-1-R	NO PARKING ANY TIME (Right Arrow)	18 X 24	3.00	—	1	—	1	1	
478+10	LT	8-24	—	Bayfield Fish Hatchery (Right arrow)	66 X 36	16.50	—	—	2	1	2	SEE SIGN DETAIL
478+10	LT	8-25	R7-1-L	NO PARKING ANY TIME (Left Arrow)	18 X 24	3.00	—	1	—	1	1	ON INSIDE POST OF SIGN NO. 8-24
478+85	LT	8-26	—	Left Curve symbol	—	—	—	—	—	1	1	
478+95	LT	8-27	W5-2	NARROW BRIDGE	36 X 36	—	9.00	—	1	1	1	
479+63	LT	8-28	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	HATCHERY RD.
480+05	RT	8-29	R4-3	SLOWER TRAFFIC KEEP RIGHT	24 X 30	5.00	—	1	—	—	—	
484+00	RT	8-30	W1-2R	Right Curve symbol	30 X 30	—	6.25	—	1	1	1	
488+00	LT	8-31	D1-1	Hatchery Rd (Right arrow)	84 X 15	8.75	—	—	2	1	2	SEE SIGN DETAIL
488+14	RT	8-32	—	SLOWER TRAFFIC KEEP RIGHT	—	—	—	—	—	1	1	
497+67	RT	8-33	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	PORT SUPERIOR RD.
501+56	LT	8-34	W1-2L	Left Curve symbol	30 X 30	—	6.25	—	1	—	—	
505+45	RT	8-35	W14-3	NO PASSING ZONE	48 X 36	—	6.00	—	1	1	1	SIGN FACES SOUTHBOUND TRAFFIC
505+50	LT	8-36	—	Left Curve symbol	—	—	—	—	—	1	1	
506+00	LT	8-37	—	Bayfield Fish Hatchery 1/2 MILE	66 X 36	16.50	—	—	2	—	—	NEW LOCATION - SEE SIGN DETAIL
511+50	RT	8-38	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	PIKES BAY RD.
512+24	LT	8-39	—	Bayfield Fish Hatchery 1/2 MILE	—	—	—	—	—	1	2	REMOVE AT EXISTING LOCATION
513+97	RT	8-40	S3-1	School Bus Stop Ahead symbol	36 X 36	—	9.00	—	1	1	1	
521+67	LT	9-1	I55-56	BLUE VISTA FARM	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
522+08	RT	9-2	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	CHEQUAMEGON RD.
522+42	LT	9-3	—	ONCOMING TRAFFIC USES CENTER LANE	—	—	—	—	—	1	1	
522+90	RT	9-4	D1-1	Weber Rd (Left arrow)	72 X 15	7.50	—	—	2	1	2	SEE SIGN DETAIL
527+20	LT	9-5	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	WEBER RD.
528+30	RT	9-6	W9-1R	RIGHT LANE ENDS	36 X 36	—	9.00	—	1	1	1	
531+00	RT	9-7	I55-56	BAYFIELD HIGH SCHOOL HONOR SOCIETY	30 X 36	7.50	—	—	1	1	1	ADOPT-A-HIGHWAY
533+05	LT	9-8	W4-2R	Right Lane Reduction Transition symbol	36 X 36	—	9.00	—	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
533+05	RT	9-9	W4-2R	Right Lane Reduction Transition symbol	36 X 36	—	9.00	—	1	1	1	
533+10	LT	9-10	D1-1	Weber Rd (Right arrow)	72 X 15	7.50	—	—	2	1	2	SEE SIGN DETAIL
537+44	RT	9-11	D1-1	Old San Rd (Right arrow)	78 X 15	8.13	—	—	2	1	2	SEE SIGN DETAIL
537+80	LT	9-12	R3-72	ONCOMING TRAFFIC USES CENTER LANE	30 X 36	7.50	—	1	—	—	—	
545+06	RT	9-13	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	OLD SAN RD.
551+97	LT	9-14	D1-1	Old San Rd (Left arrow)	78 X 15	8.13	—	—	2	1	2	SEE SIGN DETAIL
553+65	RT	9-15	W3-5	Reduced Speed Ahead 45	36 X 36	—	9.00	—	1	1	1	
555+91	RT	9-16	W14-3	NO PASSING ZONE	48 X 36	—	6.00	—	1	1	1	SIGN FACES SOUTHBOUND TRAFFIC
557+16	LT	9-17	W14-3	NO PASSING ZONE	48 X 36	—	6.00	—	1	1	1	SIGN FACES NORTHBOUND TRAFFIC
559+60	LT	9-18	R2-1	SPEED LIMIT 55	24 X 30	5.00	—	1	—	1	1	
559+60	RT	9-19	R2-1	SPEED LIMIT 45	24 X 30	5.00	—	1	—	1	1	
560+86	RT	9-20	R1-1	STOP	30 X 30	5.18	—	—	1	1	1	BLUE WING BAY RD.
				SUBTOTALS		229.09	119.75	21	44	54	64	

3

PERMANENT SIGNING ITEMS CONT'D

STH 13 STATION (APPROX.)		SIGN NO.	SIGN CODE	SIGN MESSAGE	SIGN SIZE W X H (INCHES)	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	638.2102 MOVING SIGNS TYPE II EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	COMMENT
561+05	RT	9-21	J1-1	JCT COUNTY J	24 X 39	6.50	--- ---	— —	1	--- ---	1	1	
564+87	LT	9-22	R2-1	SPEED LIMIT 45	24 X 30	5.00	---	1	—	---	1	1	
564+87	LT	9-23	J4-1	SOUTH 13	24 X 36	6.00	---	—	1	---	---	---	
564+87	LT	9-24	M1-91	LAKE SUPERIOR CIRCLE TOUR	24 X 24	4.00	—	—	---	---	1	1	ON SAME POST AS SIGN NO. 9-23
565+63		9-25	J13-1	COUNTY J Left Arrow	24 X 45	7.50	---	—	1	---	1	1	
565+71	LT	9-26	R1-1	STOP	30 X 30	5.18	—	—	1	---	1	1	CTH J
566+63	LT	9-27	J13-1	COUNTY J Right Arrow	24 X 45	7.50	—	—	1	---	1	1	
567+10	RT	9-28	J4-1	NORTH 13	24 X 36	6.00	—	—	1	---	1	1	
567+10	RT	9-29	M1-91	LAKE SUPERIOR CIRCLE TOUR	24 X 24	4.00	—	—	---	---	1	1	ON SAME POST AS SIGN NO. 9-28
568+10	RT	9-30	R2-1	SPEED LIMIT 45	24 X 30	5.00	—	1	—	---	1	1	
573+00	LT	9-31	J1-1	JCT COUNTY J	24 X 39	6.50	--- —	—	1	---	1	1	
575+15	LT	9-32	R2-1	SPEED LIMIT 45	24 X 30	5.00	—	1	—	---	---	---	NEW LOCATION
575+85	LT	10-1	—	SPEED LIMIT 45	—	---	—	---	—	---	1	1	REMOVE AT EXISTING LOCATION
575+88	RT	10-2	W3-5	Reduced Speed Ahead 25	36 X 36	---	9.00	---	1	---	1	1	
578+00	LT	10-3	D2-2	Washburn 12 Ashland 21	78 X 24	13.00	—	---	2	---	1	2	SEE SIGN DETAIL
579+14	RT	10-4	W1-2L	Left Curve symbol	30 X 30	---	6.25	---	1	---	---	---	
581+74	RT	10-5	—	Apostle Islands Natnl Lakeshore Visitor Ctr 1 MILE	90 X 48	30.00	—	---	2	---	1	2	SEE SIGN DETAIL
583+50	RT	10-6	R2-1	SPEED LIMIT 25	24 X 30	5.00	—	1	—	---	1	1	
583+50	LT	10-7	R2-1	SPEED LIMIT 45	24 X 30	5.00	—	1	—	---	1	1	
588+73	LT	10-8	J4-1	SOUTH 13	24 X 36	6.00	—	---	1	---	1	1	
588+73	LT	10-9	—	SCENIC BYWAY	—	---	—	---	—	1	---	—	ON SAME POST AS SIGN NO. 10-8
589+45	RT	10-10	R1-1	STOP	30 X 30	5.18	—	—	1	---	1	1	LAKESHORE DRIVE
590+60	RT	10-11	I-2-3	Bayfield POPULATION 487	60 X 24	10.00	—	2	—	---	1	1	SEE SIGN DETAIL
591+97	RT	10-12	R2-1	SPEED LIMIT 25	24 X 30	5.00	—	1	—	---	1	1	
592+70	LT	10-13	R2-1	SPEED LIMIT 25	24 X 30	5.00	—	1	—	---	1	1	
594+55	LT	10-14	W1-2R	Right Curve symbol	30 X 30	---	6.25	—	1	---	1	1	
595+48	LT	10-15	R1-1	STOP	30 X 30	5.18	—	—	1	---	1	1	9TH STREET
596+62	RT	10-16	W1-1R	Right Turn symbol	36 X 36	---	9.00	---	1	---	1	1	
599+62	LT	10-17	W1-6	Night Arrow (Left)	48 X 24	---	8.00	---	1	---	1	1	
599+66	LT	10-18	R1-1	STOP	30 X 30	5.18	—	---	1	---	1	1	PAYNE AVE.
600+25	LT	10-19	W1-6	Night Arrow (Right)	48 X 24	---	8.00	---	1	---	1	1	
				SUBTOTALS		162.72	46.50	9	21	1	27	29	
				TOTALS		973.22	426.25	47	185	1	186	227	

DELINEATOR ITEMS

STATION	-	STATION	LOCATION	633.0100 DELINEATOR POSTS STEEL EACH	633.0500 DELINEATOR REFLECTORS EACH
CAT. 0010					
55+00	-	57+00	STH 13 LT	3	3
151+00	-	153+00	STH 13 RT	3	3
345+00	-	356+00	STH 13 RT	12	12
362+00	-	376+50	STH 13 RT	16	16
382+00	-	420+00	STH 13 RT	39	39
498+00	-	512+00	STH 13 RT	15	15
504+50	-	506+00	STH 13 LT	3	3
540+00	-	545+00	STH 13 LT	6	6
585+00	-	587+00	STH 13 LT	3	3
			TOTALS	100	100

PAVEMENT MARKING EPOXY ITEMS

STATION	-	STATION	LOCATION	646.0106 PAVEMENT MARKING EPOXY 4-INCH YELLOW LF	646.2304.S PM GROOVED WET REFLECTIVE EPOXY 4-INCH WHITE LF	646.0842.S PM GROOVED CONTRAST WET REFLECTIVE EPOXY 4-INCH WHITE LF	COMMENTS
CAT. 0010							
37+00	-	602+84	RT	—	57389	—	EDGELINE
37+00	-	602+84	LT	—	57389	—	EDGELINE
37+00	-	40+15	CENTERLINE	78		PASSING ZONE	
40+15	-	51+20	CENTERLINE	1381		NB NO PASSING ZONE	
51+20	-	85+25	CENTERLINE	6810		DOUBLE YELLOW	
85+25	-	96+55	CENTERLINE	1412.5		SB NO PASSING ZONE	
96+55	-	113+65	CENTERLINE	427.5		PASSING ZONE	
113+65	-	116+20	CENTERLINE	319		NB NO PASSING ZONE	
116+20	-	132+60	CENTERLINE	3280		DOUBLE YELLOW	
132+60	-	143+60	CENTERLINE	1375		SB NO PASSING ZONE	
143+60	-	161+80	CENTERLINE	455		PASSING ZONE	
161+80	-	167+30	CENTERLINE	688		NB NO PASSING ZONE	
167+30	-	172+10	CENTERLINE	120		PASSING ZONE	
172+10	-	174+85	CENTERLINE	344		SB NO PASSING ZONE	
174+85	-	178+45	CENTERLINE	720		DOUBLE YELLOW	
178+45	-	186+15	CENTERLINE	963		NB NO PASSING ZONE	
186+15	-	201+00	CENTERLINE	2970		DOUBLE YELLOW	
201+00	-	209+90	CENTERLINE	1112.5		SB NO PASSING ZONE	
209+90	-	212+75	CENTERLINE	570		DOUBLE YELLOW	
212+75	-	217+05	CENTERLINE	537.5		NB NO PASSING ZONE	
217+05	-	219+90	CENTERLINE	71		PASSING ZONE	
219+90	-	227+90	CENTERLINE	1000		SB NO PASSING ZONE	
227+90	-	307+45	CENTERLINE	1989		PASSING ZONE	
307+45	-	318+70	CENTERLINE	1406		NB NO PASSING ZONE	
318+70	-	366+10	CENTERLINE	9480		DOUBLE YELLOW	
366+10	-	376+95	CENTERLINE	1356		SB NO PASSING ZONE	
376+95	-	385+30	CENTERLINE	209		PASSING ZONE	
385+30	-	396+15	CENTERLINE	1356		NB NO PASSING ZONE	
396+15	-	505+40	CENTERLINE	21850		DOUBLE YELLOW	
505+40	-	519+00	CENTERLINE	1700		NB NO PASSING ZONE	
519+00	-	545+40	CENTERLINE	5280		DOUBLE YELLOW	
545+40	-	555+95	CENTERLINE	1319		SB NO PASSING ZONE	
555+95	-	557+20	CENTERLINE	31		PASSING ZONE	
557+20	-	559+50	CENTERLINE	288		NB NO PASSING ZONE	
559+50	-	567+55	CENTERLINE	1006		NB NO PASSING ZONE	
567+55	-	602+84	CENTERLINE	8668		DOUBLE YELLOW	
335+48	-	349+00	RT	---	—	338	PASSING LANE SKIPS
455+50	-	458+00	RT	---	—	63	BYPASS LANE SKIPS
487+00	-	536+20	RT	---	—	1230	PASSING LANE SKIPS
		TOTALS		80572	114778	1631	

LOCATING NO-PASSING ZONES

STA	-	STA	LOCATION	648.0100 LOCATING NO-PASSING ZONES 01. 8160-03-70 MI
CAT 0010				
37+00	-	602+84	STH 13	10.72
			PROJECT TOTAL	10.72

TEMPORARY PAVEMENT MARKING PAINT 4-INCH

STATION	-	STATION	LOCATION	649.0402 TEMPORARY PAVEMENT MARKING PAINT 4-INCH LF	COMMENTS
CAT. 0010					
37+00	-	602+84	STH 13 MILLED SURFACE	74275	YELLOW CENTERLINE
37+00	-	602+84	STH 13 LOWER LAYER	74275	YELLOW CENTERLINE
37+00	-	602+84	STH 13 SMA LAYER	80572	SEE ITEM 646.0106 FOR CL LOCATIONS
335+48	-	349+00	STH 13 RT	108	WHITE PASSING LANE SKIPS
455+50	-	458+00	STH 13 RT	20	WHITE BYPASS LANE SKIPS
487+00	-	536+20	STH 13 RT	394	WHITE PASSING LANE SKIPS
			TOTAL	229644	

TRAFFIC CONTROL PORTABLE MESSAGE

LOCATION		MESSAGE	NO. OF PCMS	DAYS	643.1050 TRAFFIC CONTROL SIGNS PCMS DAY
CAT. 0010		*			
STH 13@ USH 53	EB	LANE RESTRICTIONS BAYFIELD SOUTH	1	28	28
STH 13@ USH 2	NB	LANE RESTRICTIONS WASHBURN NORTH	2	28	56
STH 13	NB	ROAD WORK AHEAD EXPECT DELAY	1	55	55
STH 13	SB	ROAD WORK AHEAD EXPECT DELAY	1	55	55
		TOTAL			194

* CHANGEABLE MESSAGE TO BE DETERMINED BY ENGINEER.

TRAFFIC CONTROL

LOCATION	643.0100 TRAFFIC CONTROL 01. 8160-03-70 EACH
CAT 0010	
STH 13	1
PROJECT TOTAL	1

TRAFFIC CONTROL DRUMS & LIGHTS

STATION	- STATION	LOCATION	NO. OF DRUMS	DAYS	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY
CAT. 0010						
37+00	- 602+84	PROJECT	55	54	2970	2970
454+00	- 459+60	BYPASS LANE	8	28	224	224
	UNDISTRIBUTED		12	54	648	648
		TOTALS			3618	3618

TRAFFIC CONTROL SIGNS

STATION	- STATION	LOCATION	NO. OF SIGNS	DAYS	643.0900 TRAFFIC CONTROL SIGNS DAY	COMMENT
CAT. 0010						
37+00	- 602+84	PROJECT	10	54	540	ADVANCE WARNING
37+00	- 602+84	PROJECT	37	26	962	MILLED SURFACES
37+00	- 602+84	ADV. INTERS.	8	28	224	MAX WIDTH WARNING
37+00	- 602+84	PROJECT	54	28	1512	SHOULDER WORK
37+00	- 602+84	PROJECT	24	54	1296	SIDE ROADS
		TOTAL			4534	

CONSTRUCTION STAKING RESURFACING REFERENCE

STATION	- STATION	LOCATION	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF
CAT. 0010			
37+00	- 232+33	STH 13	19533
233+59	- 239+05	STH 13	546
240+30	- 320+35	STH 13	8005
320+87	- 470+72	STH 13	14985
472+30	- 602+84	STH 13	13054
		TOTAL	56123

CONST. STAKING SUPPLEMENTAL CONTROL

LOCATION	650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 01.8160-03-70 LS
CAT 0010	
STH 13	1
PROJECT TOTAL	1

REMOVING ASPHALTIC SURFACE MILLING FULL DEPTH

STATION	- STATION	LOCATION	SPV.0180.01 REMOVING ASPHALTIC SURFACE MILLING FULL DEPTH SY
CAT. 0010			
37+50	- 228+05	STH 13 RT	10587
37+50	- 229+82	STH 13 LT	10685
228+05	- 232+33	STH 13 RT	286
229+82	- 232+33	STH 13 LT	168
233+59	- 239+05	STH 13 LT	364
233+59	- 234+84	STH 13 RT	84
237+82	- 239+05	STH 13 RT	82
240+30	- 248+17	STH 13 LT	525
240+30	- 245+05	STH 13 RT	317
245+05	- 248+17	STH 13 RT	174
248+17	- 319+00	STH 13 RT & LT	7870
319+00	- 322+25	STH 13 RT & LT	434
322+25	330+85	STH 13 RT & LT	956
330+85	- 337+00	STH 13 RT & LT	889
337+00	- 351+00	STH 13 RT & LT	1089
351+00	- 356+64	STH 13 RT & LT	815
356+64	- 360+82	STH 13 RT & LT	511
360+82	- 371+13	STH 13 RT & LT	1490
371+13	- 454+00	STH 13 RT & LT	9208
454+00	- 455+35	STH 13 RT & LT	195
455+35	- 458+24	STH 13 RT & LT	611
458+24	- 459+60	STH 13 RT & LT	197
459+60	- 465+95	STH 13 RT & LT	706
465+95	- 467+95	STH 13 RT & LT	245
467+95	- 470+72	STH 13 RT & LT	370
472+31	- 475+60	STH 13 RT & LT	439
475+60	- 479+00	STH 13 RT & LT	378
479+00	- 486+90	STH 13 RT & LT	1142
486+90	- 537+80	STH 13 RT & LT	3959
537+80	- 544+00	STH 13 RT & LT	896
544+00	- 590+20	STH 13 RT & LT	5134
590+20	- 592+01	STH 13 RT & LT	222
592+01	- 597+80	STH 13 RT & LT	644
597+80	- 602+34	STH 13 RT & LT	555
		TOTAL	62227

SAWING ASPHALT

LOCATION	690.0150 SAWING ASPHALT LF
CAT. 0010	
37+00 BOP	34
602+84 EOP	34
SUPERIOR AVE	24
BURLAGER RD	23
FRIENDLY VALLEY RD W	24
SKI HILL RD	26
HATCHERY RD	24
PORT SUPERIOR RD	30
PIKES BAY RD	24
CHEQUAMEGON RD	23
WEBER RD	28
OLD SAN RD	27
CTH J	29
9TH ST	28
PAYNE AVE	22
DRIVEWAYS	420
UNDISTRIBUTED	60
TOTAL	880

MATERIAL TRANSFER VEHICLE

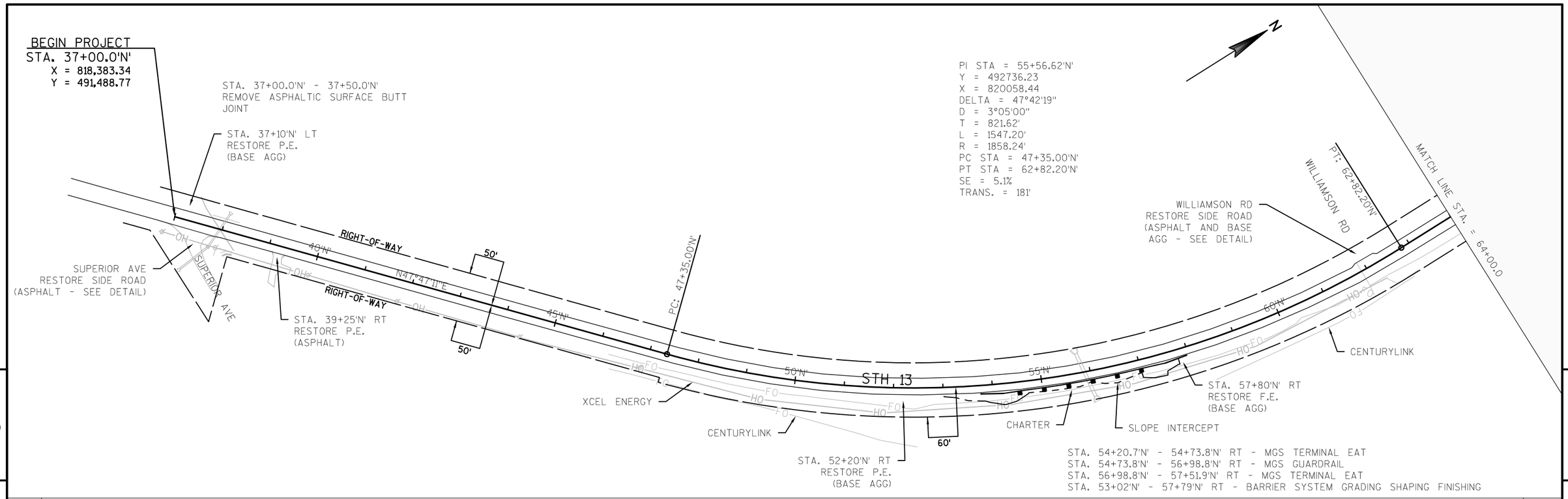
LOCATION	SPV.0105.01 MATERIAL TRANSFER VEHICLE LS
CAT 0010	
STH 13	1
PROJECT TOTAL	1

SMA PAVEMENT COMPACTION

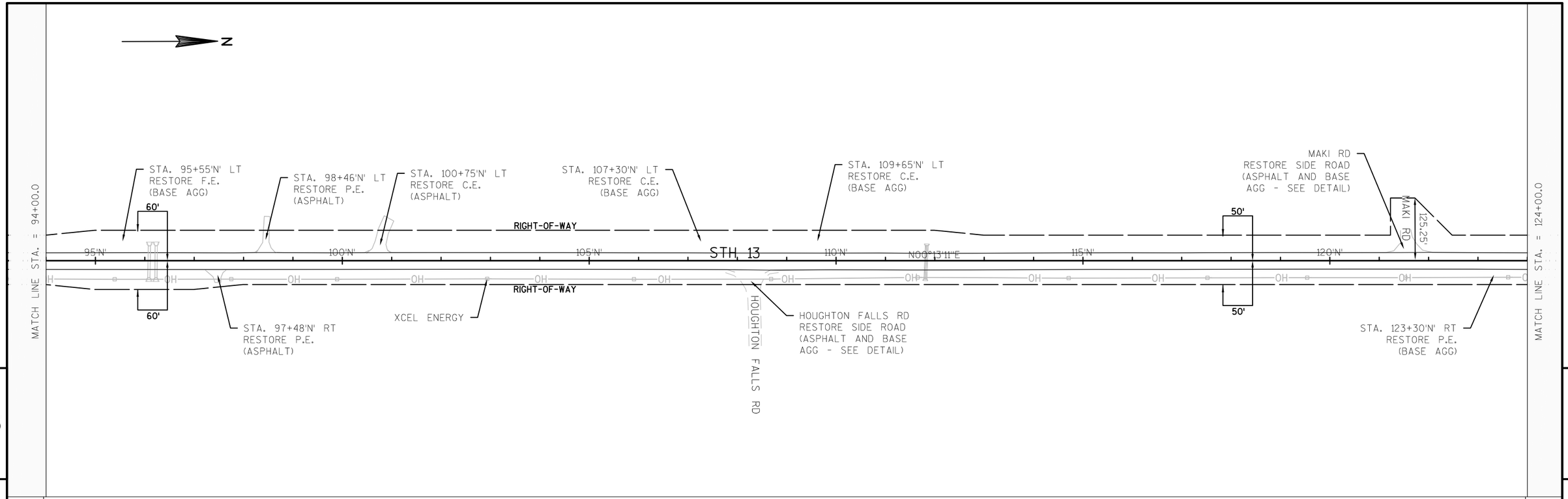
LOCATION	SPV.0195.01 SMA PAVEMENT COMPACTION ACCEPTANCE TON
CAT 0010	
STH 13	15342
TOTAL	15342

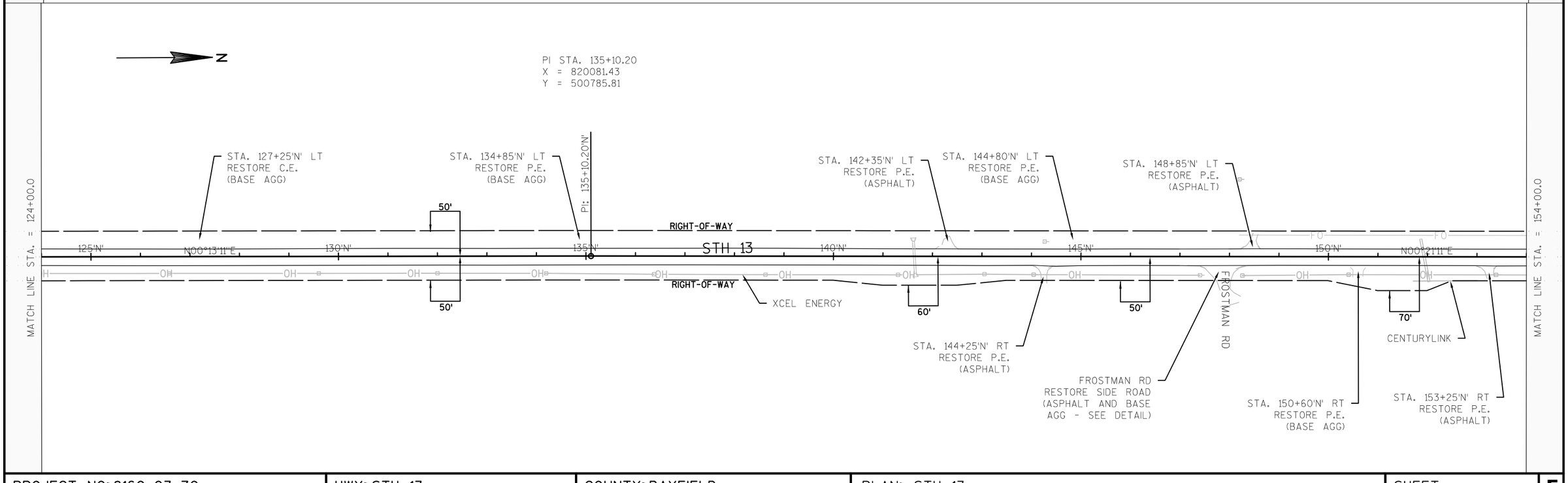
MILLING AND REMOVING TEMPORARY JOINT

LOCATION	SPV.0105.02 MILLING AND REMOVING TEMPORARY JOINT LS
CAT 0010	
STH 13	1
PROJECT TOTAL	1

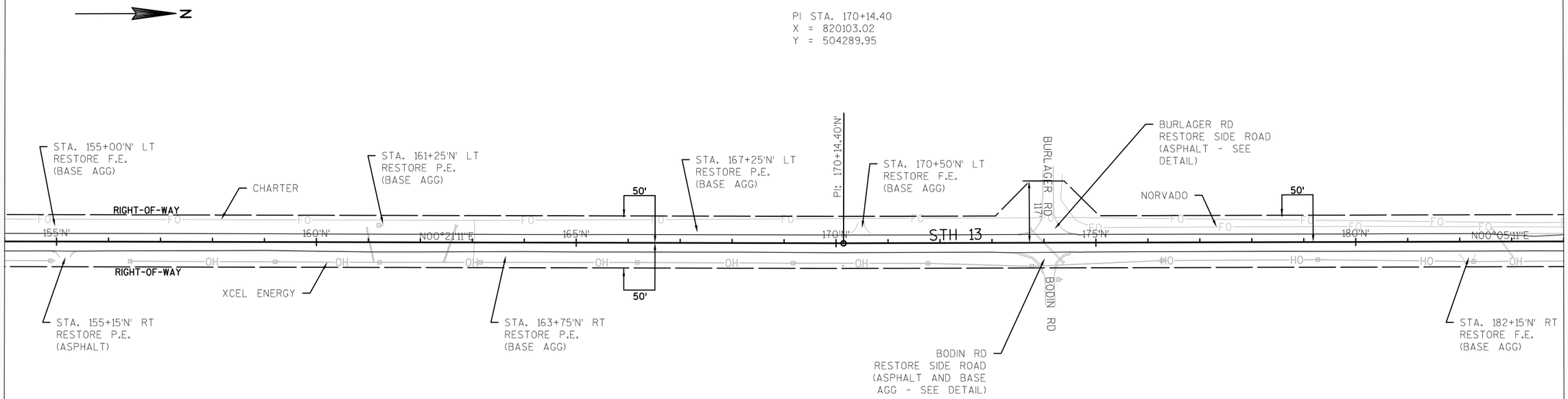


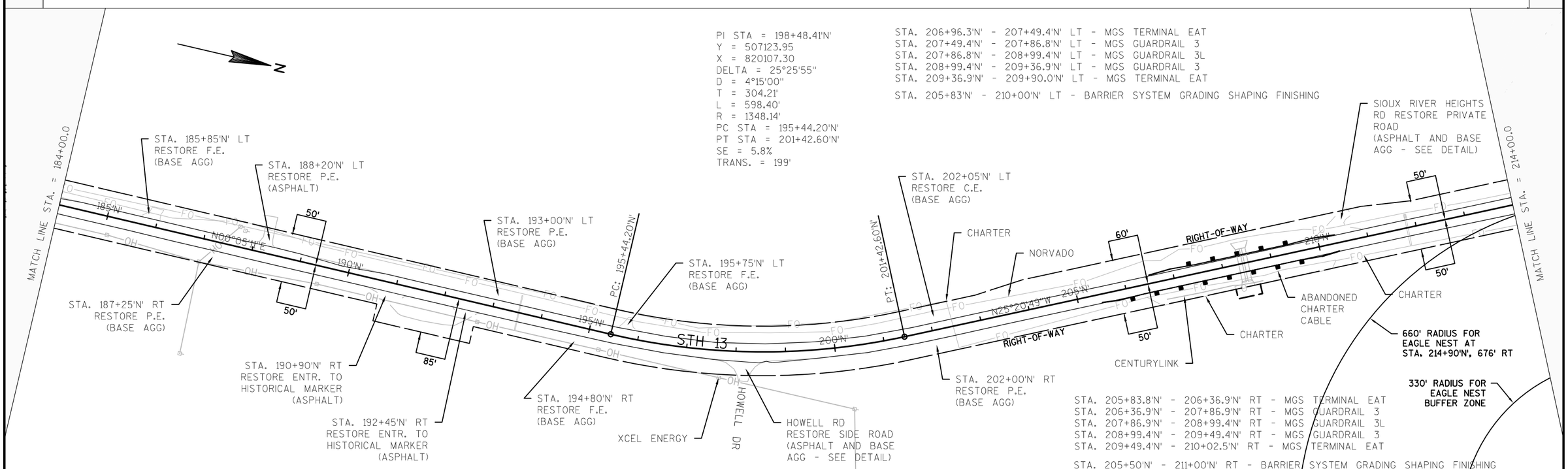
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 48 E





PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 49	E





PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 50	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\050201_PN_STH13.DWG
LAYOUT NAME - STH13_PN-5
PLOT DATE : 1/26/2017 2:17 PM
PLOT BY : USER
PLOT NAME :
PLOT SCALE : 1 IN:200 FT
WISDOT/CADDS SHEET 44

STH 13 LINE TABLE	
LINE NO.	BEARING
L1	N25° 20' 49"W

STA. 230+05'N' - 232+24'N' LT - REMOVING GUARDRAIL

STA. 229+81.6'N' - 230+34.7'N' LT - MGS TERMINAL EAT
STA. 230+34.7'N' - 231+84.7'N' LT - MGS GUARDRAIL 3
STA. 231+84.7'N' - 232+24.1'N' LT - MGS THRIE BEAM

STA. 233+72'N' - 234+77'N' LT - REMOVING GUARDRAIL
STA. 237+90'N' - 239+99'N' LT - REMOVING GUARDRAIL

STA. 233+72.3'N' - 234+11.7'N' LT - MGS THRIE BEAM
STA. 234+11.7'N' - 234+24.2'N' LT - MGS GUARDRAIL 3
STA. 234+24.2'N' - 238+46.8'N' LT - MGS GUARDRAIL 3K
STA. 238+46.8'N' - 238+59.3'N' LT - MGS GUARDRAIL 3
STA. 238+59.3'N' - 238+98.7'N' LT - MGS THRIE BEAM

STA. 228+40'N' - 250+00'N' LT - BARRIER SYSTEM GRADING SHAPING FINISHING

MATCH LINE STA. = 214+00.0

PC: 214+51.30'N

PT: 221+49.60'N

NORVADO

STA. 227+00'N' LT
RESTORE WAYSIDE
(BASE AGG)

STA. 229+50'N' LT
RESTORE WAYSIDE
(BASE AGG)

EXCEPTION TO NET ℄ LENGTH
STA. 232+33.13'N' - STA. 233+58.7'N'
STRUCTURE B-4-098 (SIOUX RIVER)

EXCEPTION TO NET ℄ LENGTH
STA. 239+05.2'N' - STA. 240+30.1'N'
STRUCTURE B-4-099 (SIOUX RIVER)

RIGHT-OF-WAY

50'

100'

215'N

220'N

225'N

230'N

235'N

240'N

STH 13

N16°36'49"W

RIGHT-OF-WAY

50'

50'

CHARTER

PI STA = 218+01.13'N
Y = 508897.75
X = 819267.06
DELTA = 8°43'44"
D = 1°15'00"
T = 349.83'
L = 698.30'
R = 4583.66'
PC STA = 214+51.30'N
PT STA = 221+49.60'N
SE = 2.8%
TRANS. = 122'

660' RADIUS FOR
EAGLE NEST AT
STA. 214+90'N', 676' RT

330' RADIUS FOR
EAGLE NEST
BUFFER ZONE

STA. 231+68'N' - 232+18'N'
REMOVE ASPHALTIC SURFACE BUTT
JOINT

STA. 231+26'N' - 232+28'N' RT - REMOVING GUARDRAIL

STA. 231+26.0'N' - 231+88.5'N' RT - MGS GUARDRAIL 3
STA. 231+88.5'N' - 232+27.9'N' RT - MGS THRIE BEAM

EQUATION

STA BACK = 232+94.41'N
STA AHEAD = 232+95.00'N

STA. 233+74'N' - 234+24'N'
REMOVE ASPHALTIC SURFACE
BUTT JOINT

STA. 237+60'N' RT
RESTORE BOAT ACCESS
(BASE AGG)

STA. 238+39'N' - 238+89'N'
REMOVE ASPHALTIC SURFACE
BUTT JOINT

STA. 240+46'N' - 240+96'N'
REMOVE ASPHALTIC SURFACE
BUTT JOINT

STA. 233+66'N' - 234+64'N' RT - REMOVING GUARDRAIL

STA. 233+66.3'N' - 234+05.7'N' RT - MGS THRIE BEAM
STA. 234+05.7'N' - 234+30.7'N' RT - MGS GUARDRAIL 3
STA. 234+30.7'N' - 234+83.8'N' RT - MGS TERMINAL EAT

STA. 237+91'N' - 239+00'N' RT - REMOVING GUARDRAIL

STA. 237+82.5'N' - 238+35.6'N' RT - MGS TERMINAL EAT
STA. 238+35.6'N' - 238+60.6'N' RT - MGS GUARDRAIL 3
STA. 238+60.6'N' - 239+00.0'N' RT - MGS THRIE BEAM

STA. 231+26'N' - 246+24'N' RT - BARRIER SYSTEM GRADING SHAPING FINISHING

MATCH LINE STA. = 244+00.6

STA. 240+50'N' - 241+60'N' LT - REMOVING GUARDRAIL

STA. 240+49.8'N' - 240+89.2'N' LT - MGS THRIE BEAM
STA. 240+89.2'N' - 247+64.2'N' LT - MGS GUARDRAIL 3
STA. 247+64.2'N' - 248+17.3'N' LT - MGS TERMINAL EAT

PI STA = 253+64.90'N
Y = 512313.47
X = 818247.87
DELTA = 5°48'12"
D = 0°50'00"
T = 348.50'
L = 696.40'
R = 6875.49'
PC STA = 250+16.40'N
PT STA = 257+12.80'N
SE = 2.0%
TRANS. = 102'

SLOPE REPAIR STATION
247+00 -249+10'N' LT

FRIENDLY VALLEY RD

FRIENDLY VALLEY RD
RESTORE SIDE ROAD
(ASPHALT - SEE
DETAIL)

BAYFIELD
ELECTRIC

KJARVICK RD

KJARVICK RD
RESTORE SIDE ROAD
(ASPHALT AND BASE
AGG - SEE DETAIL)

STA. 270+15'N' LT
RESTORE P.E.
(BASE AGG)

NORVADO

RIGHT-OF-WAY

50'

50'

50'

43'

28'

43'

215'N

N16°36'49"W

250'N

255'N

260'N

265'N

270'N

N22°24'49"W

STH 13

RIGHT-OF-WAY

CHARTER

PC: 250+16.40'N

PT: 257+12.80'N

FRIENDLY VALLEY RD

STA. 245+25'N' RT
RESTORE F.E.
(BASE AGG)

STA. 249+10'N' LT
RESTORE F.E.
(BASE AGG)

FRIENDLY VALLEY RD
RESTORE SIDE ROAD
(ASPHALT AND BASE
AGG - SEE DETAIL)

STA. 256+35'N' RT
RESTORE F.E.
(BASE AGG)

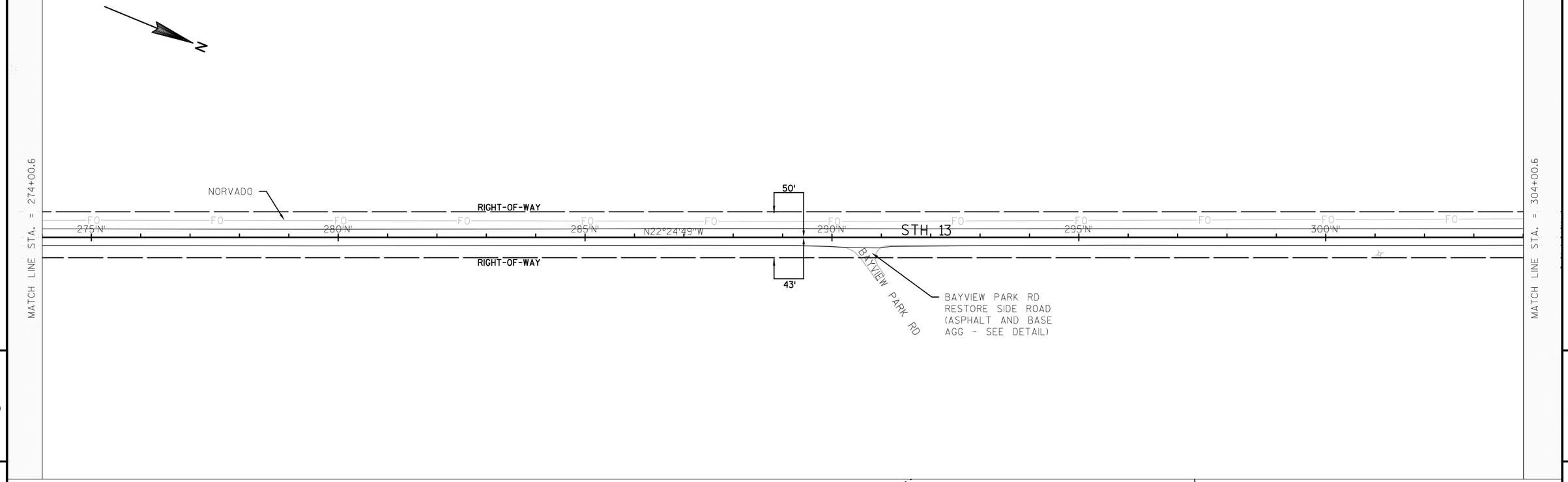
STA. 272+00'N' RT
RESTORE F.E.
(BASE AGG)

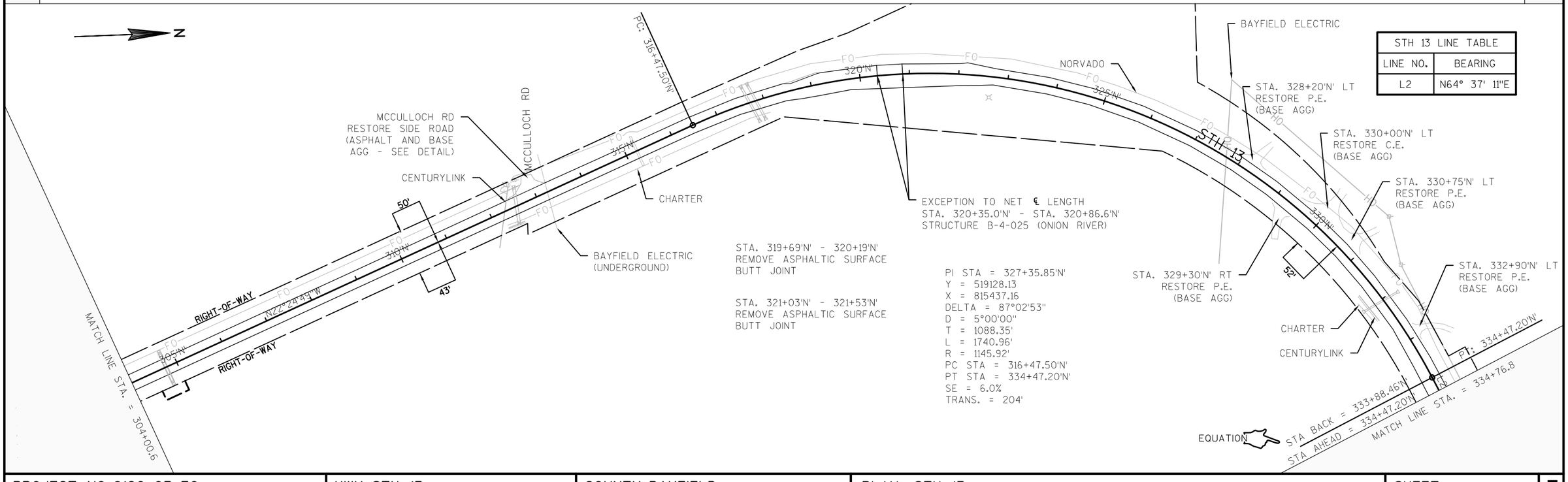
STA. 240+37'N' - 241+48'N' RT - REMOVING GUARDRAIL

STA. 240+37.6'N' - 240+77.0'N' RT - MGS THRIE BEAM
STA. 240+77.0'N' - 244+52.0'N' RT - MGS GUARDRAIL 3
STA. 244+52.0'N' - 245+05.1'N' RT - MGS TERMINAL EAT

MATCH LINE STA. = 274+00.6

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 51	E



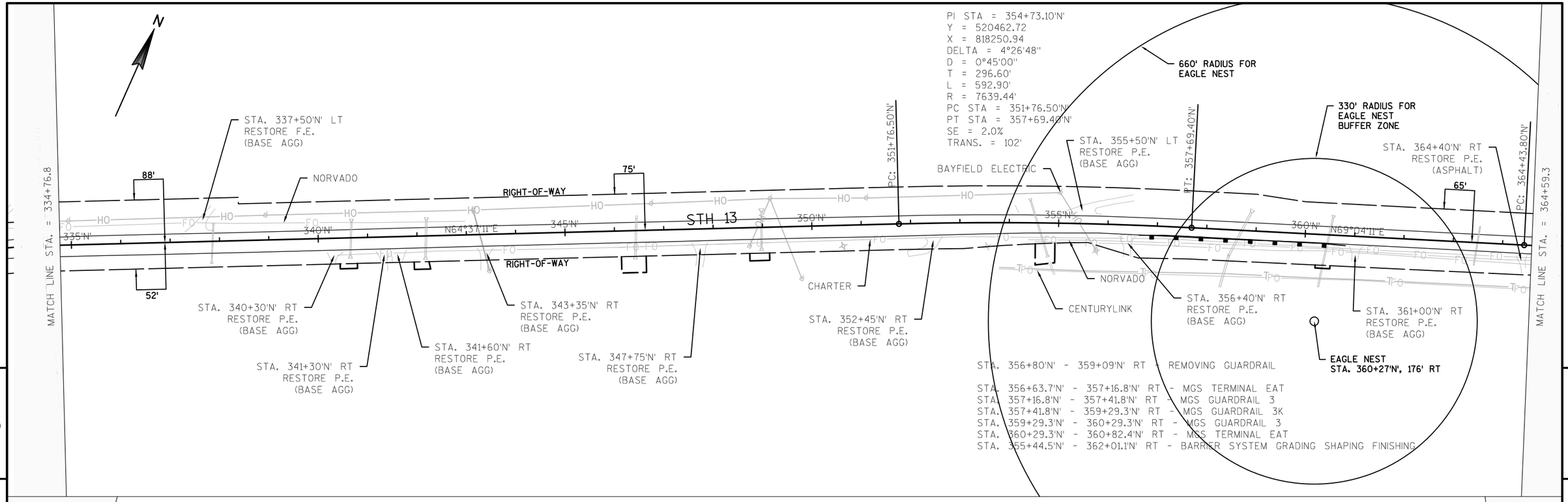


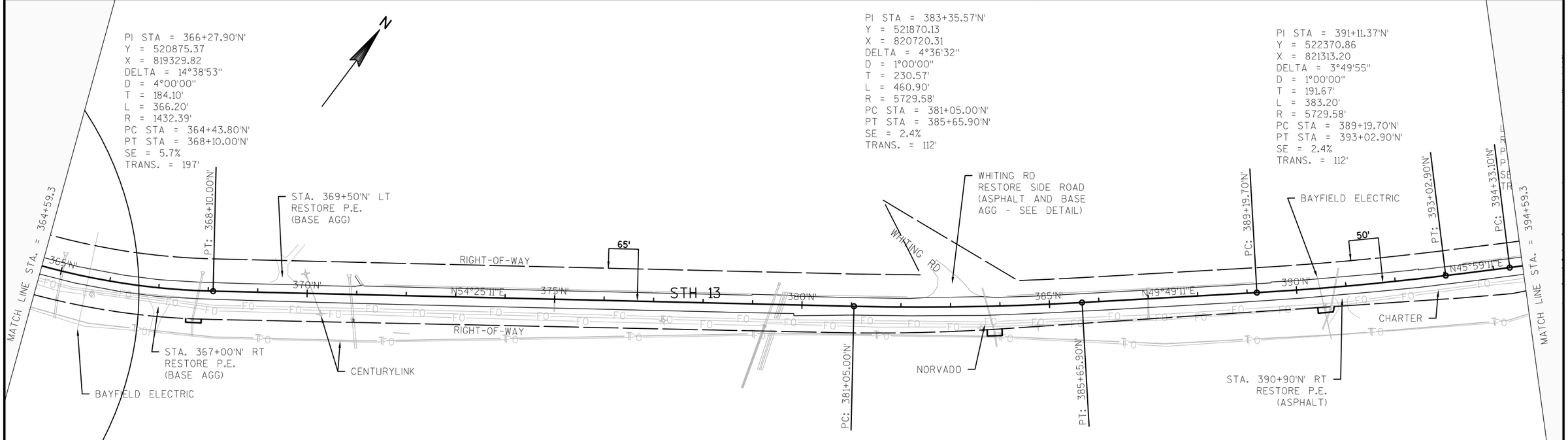
STH 13 LINE TABLE	
LINE NO.	BEARING
L2	N64° 37' 11"E

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 52	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\050201_PN_STH13.DWG
LAYOUT NAME - STH13_PN-9
PLOT DATE : 1/26/2017 2:21 PM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:200 FT

WISDOT/CADDS SHEET 44





PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 53	E

PI STA = 396+14.30'N'
Y = 522720.41
X = 821675.00
DELTA = 9°56'28"
D = 2°45'00"
T = 181.20'
L = 361.50'
R = 2083.48'
PC STA = 394+33.10'N'
PT STA = 397+94.60'N'
SE = 4.8%
TRANS. = 174'

PI STA = 405+17.19'N'
Y = 523451.24
X = 822206.72
DELTA = 2°26'58"
D = 0°30'00"
T = 244.99'
L = 489.90'
R = 11459.16'
PC STA = 402+72.20'N'
PT STA = 407+62.10'N'
SE = NORMAL CROWN

PI STA = 409+36.73'N'
Y = 523800.80
X = 822438.85
DELTA = 11°51'36"
D = 5°00'00"
T = 119.03'
L = 237.20'
R = 1145.92'
PC STA = 408+17.70'N'
PT STA = 410+54.90'N'
SE = 6.0%
TRANS. = 204'

PI STA = 414+54.35'N'
Y = 524282.41
X = 822630.84
DELTA = 4°39'55"
D = 1°20'00"
T = 175.05'
L = 349.90'
R = 4297.18'
PC STA = 412+79.30'N'
PT STA = 416+29.20'N'
SE = 3.0%
TRANS. = 128'

PI STA = 419+35.11'N'
Y = 524742.18
X = 822772.02
DELTA = 10°32'29"
D = 2°40'00"
T = 198.21'
L = 395.30'
R = 2148.59'
PC STA = 417+36.90'N'
PT STA = 421+32.20'N'
SE = 4.7%
TRANS. = 171'

MATCH LINE STA. = 394+59.3

395'N'

PT: 397+94.60'N'

400'N'

N36°02'11"E

PC: 402+72.20'N'

50'

RIGHT-OF-WAY

405'N'

STH 13

PT: 407+62.10'N'

PC: 408+17.70'N'

PT: 410+54.90'N'

410'N'

N21°44'11"E

PC: 412+79.30'N'

STA. 413+28'N' LT
RESTORE P.E.
(BASE AGG)

EXIST CURB INLET
(NO AEW)

PT: 416+29.20'N'

PC: 417+36.90'N'

415'N'

N17°04'11"E

PT: 421+32.20'N'

PC: 423+52.00'N'

N06°31'11"E

420'N'

MATCH LINE STA. = 424+59.3

STA. 399+05'N' RT
RESTORE P.E.
(BASE AGG)

CENTURYLINK

STA. 408+00'N' RT
RESTORE P.E.
(BASE AGG)

CHARTER

NORVADO

STA. 412+70'N' RT
RESTORE P.E.
(BASE AGG)

50'

BAYFIELD ELECTRIC

STA. 421+10'N' RT
RESTORE P.E.
(BASE AGG)

5

5

PI STA = 426+79.50'N'
Y = 525482.86
X = 822856.70
DELTA = 9°48'04"
D = 1°30'00"
T = 327.50'
L = 653.40'
R = 3819.72'
PC STA = 423+52.00'N'
PT STA = 430+08.70'N'
SE = 3.3%
TRANS. = 135'

STH 13 LINE TABLE	
LINE NO.	BEARING
L3	NO3° 16' 49"W

PI STA = 435+89.58'N'
Y = 526389.76
X = 822804.72
DELTA = 18°58'34"
D = 2°00'00"
T = 478.78'
L = 948.80'
R = 2864.79'
PC STA = 431+10.80'N'
PT STA = 440+59.60'N'
SE = 4.0%
TRANS. = 153'

PI STA = 444+01.39'N'
Y = 527149.22
X = 822493.98
DELTA = 9°18'43"
D = 2°40'00"
T = 174.99'
L = 349.20'
R = 2148.59'
PC STA = 442+26.40'N'
PT STA = 445+75.60'N'
SE = 4.7%
TRANS. = 171'

MATCH LINE STA. = 424+59.3

S. KENNEDY RD
GRADING SHAPING AND FINISHING
INTERSECTION. PLACE 4-INCH
ASPHALT SURFACE. (ASPHALT AND
BASE AGG - SEE DETAIL)

425'N'

S. KENNEDY RD

PT: 430+08.70'N'

PC: 431+10.80'N'

50'

436'N' L3

RIGHT-OF-WAY

435'N'

STH 13

440'N'

N22°44'49"W

445'N'

N31°33'49"W

PT: 440+59.60'N'

55'

PC: 442+26.40'N'

PT: 445+75.60'N'

STA. 438+85'N' LT
RESTORE P.E.
(ASPHALT)

STA. 443+55'N' LT
RESTORE P.E.
(BASE AGG)

STA. 446+10'N' LT
RESTORE C.E.
(ASPHALT)

PC: 448+80.20'N'

STA. 451+95'N' LT
RESTORE F.E.
(BASE AGG)

XCEL ENERGY

STA. 450+25'N' LT
RESTORE F.E.
(BASE AGG)

PT: 452+56.60'N'

450'N'

N13°59'49"W

CENTURYLINK

CHARTER

NORVADO

STA. 450+65'N' RT
RESTORE P.E.
(ASPHALT)

STA. 454+00'N' RT
RESTORE P.E.
(BASE AGG)

SPBG SHORT RADIUS-RAD. PT.
449+86.4, 41.8' RT

STA. 450+25'N' RT
RESTORE P.E.
(BASE AGG)

SLOPE INTERCEPT
(TYP.)

MATCH LINE STA. = 454+54.2

STA. 428+90'N' RT
RESTORE P.E.
(ASPHALT)

CENTURYLINK

STA BACK = 430+05.40'N'
STA AHEAD = 430+08.70'N'

SPBG SHORT RADIUS-RAD. PT.
437+62.7, 35.3' RT

SPBG SHORT RADIUS-RAD. PT.
438+27.3, 37.9' RT

STA. 437+95'N' RT
RESTORE P.E.
(BASE AGG)

PI STA = 450+69.89'N'
Y = 527719.48
X = 822143.66
DELTA = 17°33'55"
D = 4°40'00"
T = 189.69'
L = 376.40'
R = 1227.77'
PC STA = 448+80.20'N'
PT STA = 452+56.60'N'
SE = 6.0%
TRANS. = 204'

EQUATION

STA. 429+75.2'N' - 430+25.2'N' RT - SPBG ENERGY ABSORBING TERMINAL
STA. 430+25.2'N' - 437+56.5'N' RT - SPBG CLASS 'A'
STA. 437+56.5'N' - 437+80.0'N' RT - SPBG SHORT RADIUS (17.25' RAD, 7 CRT POSTS)
STA. 437+80.0'N' - 437+78.2'N' RT - SPBG SHORT RADIUS TERMINAL

STA. 438+01.2'N' - 438+04.9'N' RT - SPBG SHORT RADIUS TERMINAL
STA. 438+04.6'N' - 438+33.6'N' RT - SPBG SHORT RADIUS (19.9' RAD., 7 CRT POSTS)
STA. 438+33.6'N' - 449+83.6'N' RT - SPBG CLASS 'A'
STA. 449+83.6'N' - 450+11.0'N' RT - SPBG SHORT RADIUS (24.0' RAD., 9 CRT POST)
STA. 450+11.0'N' - 450+05.8'N' RT - SPBG SHORT RADIUS TERMINAL

STA. 429+00'N' - 450+06'N' RT - BARRIER SYSTEM GRADING SHAPING FINISHING

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 54	E

STH 13 LINE TABLE	
LINE NO.	BEARING
L4	N13° 59' 49"W

STA. 468+33'N' - 470+62'N' LT - REMOVING GUARDRAIL

STA. 465+94.9'N' - 466+44.9'N' LT - MGS TERMINAL EAT
STA. 466+44.9'N' - 468+48.0'N' LT - MGS GUARDRAIL 3
STA. 468+48.0'N' - 469+48.0'N' LT - MGS GUARDRAIL 3K
STA. 469+48.0'N' - 470+23.0'N' LT - MGS GUARDRAIL 3
STA. 470+23.0'N' - 470+62.4'N' LT - MGS THRIE BEAM TRANSITION

STA. 464+10'N' - 476+78'N' LT - BARRIER SYSTEM GRADING SHAPING FINISHING

STA. 470+22'N' - 470+72'N'
REMOVE ASPHALTIC SURFACE BUTT JOINT

EXCEPTION TO NET ℄ LENGTH
STA. 470+71.9'N' - STA. 472+30.6'N'
STRUCTURE B-4-009 (PIKES CREEK)

STA. 472+31'N' - 472+81'N'
REMOVE ASPHALTIC SURFACE BUTT JOINT

STA. 472+17'N' - 474+17'N' LT - REMOVING GUARDRAIL

STA. 472+16.9'N' - 472+56.3'N' LT - MGS THRIE BEAM TRANSITION
STA. 472+56.'N'3 - 473+43.8'N' LT - MGS GUARDRAIL 3
STA. 473+43.8'N' - 474+43.8'N' LT - MGS L (STA. 473+84.4'N'-473+96.9'N, POST 1 TO POST 1)
STA. 474+43.8'N' - 475+06.3'N' LT - MGS GUARDRAIL 3
STA. 475+06.3'N' - 475+59.4'N' LT - MGS TERMINAL EAT

N

SKI HILL RD
RESTORE SIDE ROAD
(ASPHALT - SEE
DETAIL)

XCEL ENERGY

STA. 463+85'N' LT
RESTORE P.E.
(BASE AGG)

PT: 467+06.90'N'

PC: 468+19.20'N'

PT: 470+99.30'N'

STA. 477+15'N' LT
RESTORE C.E.
(ASPHALT)

MILLIGAN RD

HATCHERY RD
RESTORE SIDE ROAD
(ASPHALT - SEE
DETAIL)

HATCHERY RD

XCEL ENERGY
GAS

RIGHT-OF-WAY

STH 13

SKI HILL RD

MATCH LINE STA. = 454+54.2

MATCH LINE STA. = 485+46.8

55'

50'

65'

55'

50'

460'N'

465'N'

470'N'

480'N'

485'N'

N20°24'11"E

N33°28'11"E

N32°41'11"E

CENTURYLINK
(AERIAL)

XCEL ENERGY
GAS

CHARTER

STA. 461+40'N' RT
RESTORE C.E.
(ASPHALT)

STA. 464+10'N' RT
RESTORE C.E.
(ASPHALT)

STA. 466+20'N' RT
RESTORE P.E.
(BASE AGG)

PCC: 459+82.50'N

NORVADO

STA. 477+05'N' RT
RESTORE F.E.
(BASE AGG)

STA BACK = 471+10.30'N'
STA AHEAD = 471+94.50'N'

EQUATION

PI: 478+00.00'N'

PI STA. 479+46.83
X = 822677.95
Y = 530191.12

STA. 469+63'N' - 470+88'N' RT - REMOVING GUARDRAIL

STA. 467+95.2'N' - 468+48.3'N' RT - MGS TERMINAL EAT
STA. 468+48.3'N' - 470+48.3'N' RT - MGS GUARDRAIL 3
STA. 470+48.3'N' - 470+87.7'N' RT - MGS THRIE BEAM TRANSITION

STA. 472+43'N' - 473+66'N' RT - REMOVING GUARDRAIL

STA. 472+42.7'N' - 472+82.1'N' RT - MGS THRIE BEAM TRANSITION
STA. 472+82.1'N' - 474+19.6'N' RT - MGS GUARDRAIL 3
STA. 474+19.6'N' - 474+72.7'N' RT - MGS TERMINAL EAT

STA. 466+66'N' - 475+92'N' RT - BARRIER SYSTEM GRADING SHAPING FINISHING

PI STA = 457+36.14'N'
Y = 528368.84
X = 821981.79
DELTA = 19°54'43"
D = 4°00'00"
T = 251.44'
L = 497.80'
R = 1432.39'
PC STA = 454+84.70'N'
PT STA = 459+82.50'N'
SE = 5.7%
TRANS. = 197'

PI STA = 463+46.64'N'
Y = 528981.14
X = 822045.23
DELTA = 14°29'17"
D = 2°00'00"
T = 364.14'
L = 724.40'
R = 2864.79'
PC STA = 459+82.50'N'
PT STA = 467+06.90'N'
SE = 4.0%
TRANS. = 153'

PI STA = 469+59.86'N'
Y = 529559.52
X = 822260.37
DELTA = 13°04'17"
D = 4°40'00"
T = 140.66'
L = 280.10'
R = 1227.77'
PC STA = 468+19.20'N'
PT STA = 470+99.30'N'
SE = 6.0%
TRANS. = 204'

N

STA. 494+00'N' LT
RESTORE F.E.
(BASE AGG)

XCEL ENERGY

NORVADO

PT: 498+30.10'N'

STA. 501+05'N' LT
RESTORE P.E.
(BASE AGG)

CENTURYLINK

CHARTER
(COAX.)

NORVADO

STA. 511+68'N' LT
RESTORE P.E.
(ASPHALT & BASE
AGG)

STA. 513+15'N' LT
RESTORE F.E.
(BASE AGG)

XCEL ENERGY
GAS

RIGHT-OF-WAY

RIGHT-OF-WAY

STH 13

MATCH LINE STA. = 485+46.8

MATCH LINE STA. = 515+42.0

50'

65'

60'

80'

70'

62'

60'

490'N'

495'N'

500'N'

505'N'

510'N'

515'N'

N32°41'11"E

N56°05'11"E

PC: 486+60.40'N

CHARTER

NORVADO

STA. 495+85'N' RT
RESTORE F.E.
(BASE AGG)

PORT SUPERIOR RD
RESTORE SIDE ROAD
(ASPHALT - SEE DETAIL)

PORT SUPERIOR RD

EQUATION

STA BACK = 498+34.90'N'
STA AHEAD = 498+30.10'N'

CHARTER

STA. 511+30'N' RT
RESTORE C.E.
(ASPHALT)

OAKES BAY RD

XCEL ENERGY

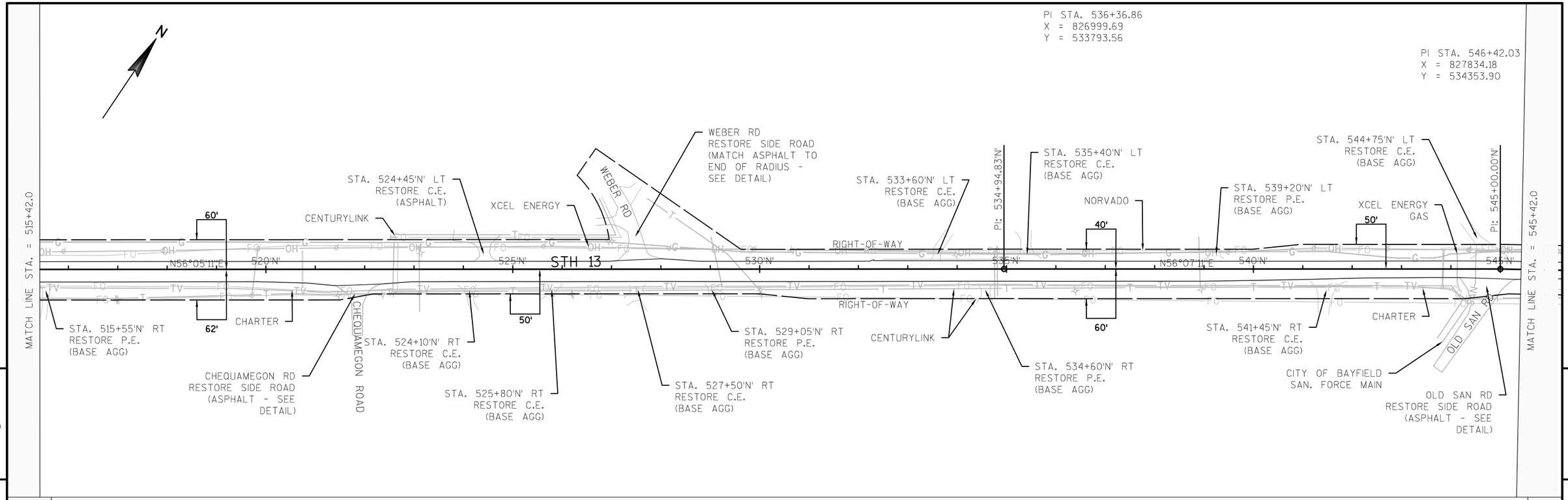
CENTURYLINK

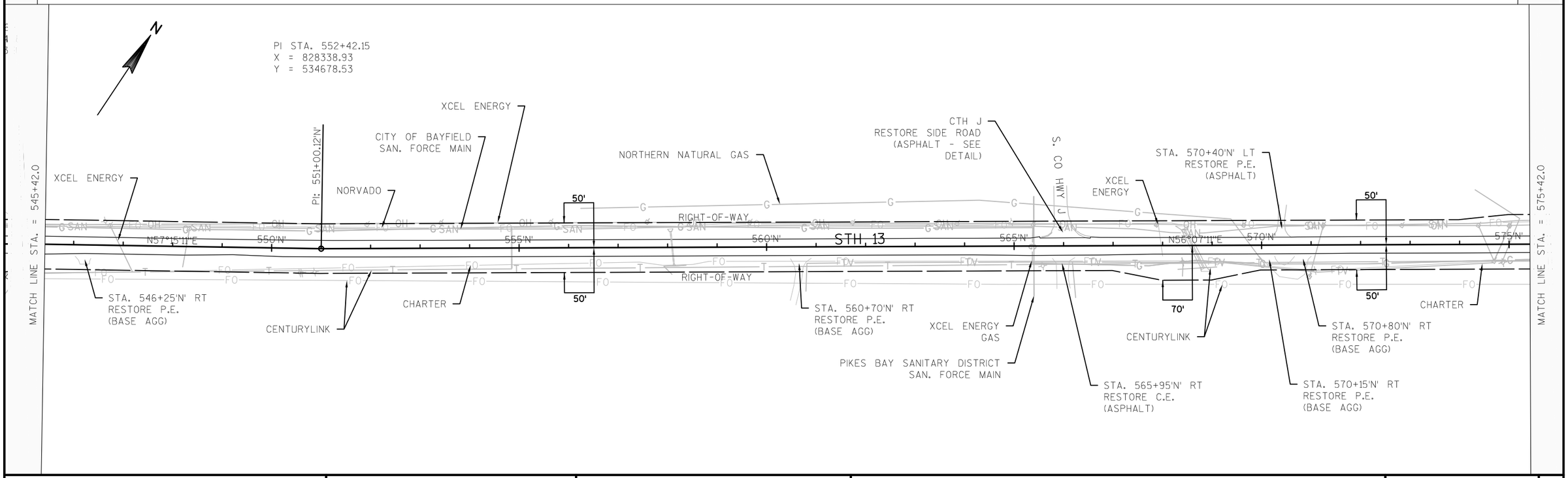
PI STA = 492+55.95'N'
Y = 531416.50
X = 823464.22
DELTA = 23°23'32"
D = 1°59'30"
T = 595.55'
L = 1174.50'
R = 2876.78'
PC STA = 486+60.40'N'
PT STA = 498+30.10'N'
SE = 4.0%
TRANS. = 153'

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 55	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\050201_PN_STH13.DWG
LAYOUT NAME - STH13_PN-15
PLOT DATE : 3/8/2017 10:27 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:200 FT

WISDOT/CADDS SHEET 44



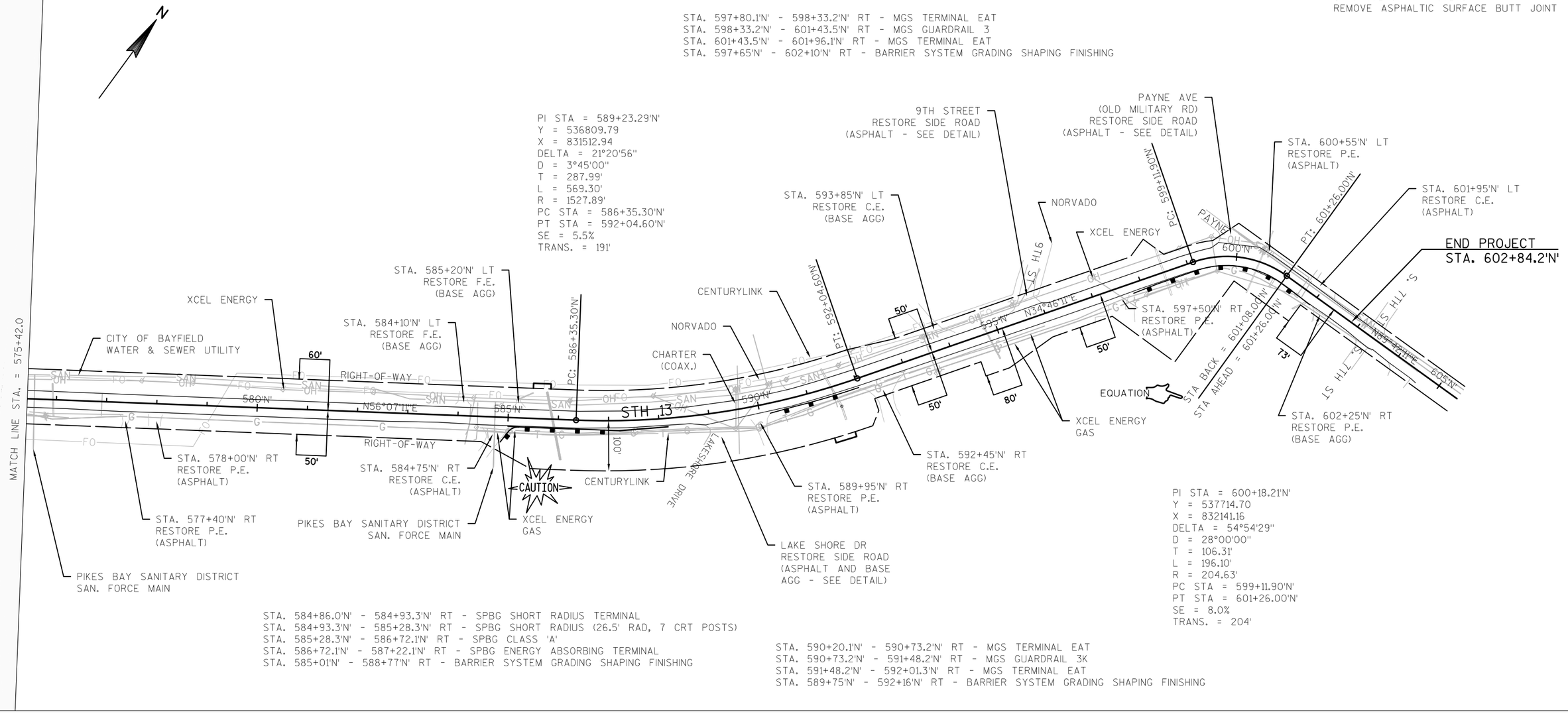


PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 56	E

STA. 598+99'N' - 601+98'N' RT - REMOVING GUARDRAIL

STA. 597+80.1'N' - 598+33.2'N' RT - MGS TERMINAL EAT
STA. 598+33.2'N' - 601+43.5'N' RT - MGS GUARDRAIL 3
STA. 601+43.5'N' - 601+96.1'N' RT - MGS TERMINAL EAT
STA. 597+65'N' - 602+10'N' RT - BARRIER SYSTEM GRADING SHAPING FINISHING

STA. 602+34'N' - 602+84'N'
REMOVE ASPHALTIC SURFACE BUTT JOINT



PI STA = 600+18.21'N'
Y = 537714.70
X = 832141.16
DELTA = 54°54'29"
D = 28°00'00"
T = 106.31'
L = 196.10'
R = 204.63'
PC STA = 599+11.90'N'
PT STA = 601+26.00'N'
SE = 8.0%
TRANS. = 204'

STA. 584+86.0'N' - 584+93.3'N' RT - SPBG SHORT RADIUS TERMINAL
STA. 584+93.3'N' - 585+28.3'N' RT - SPBG SHORT RADIUS (26.5' RAD, 7 CRT POSTS)
STA. 585+28.3'N' - 586+72.1'N' RT - SPBG CLASS 'A'
STA. 586+72.1'N' - 587+22.1'N' RT - SPBG ENERGY ABSORBING TERMINAL
STA. 585+01'N' - 588+77'N' RT - BARRIER SYSTEM GRADING SHAPING FINISHING

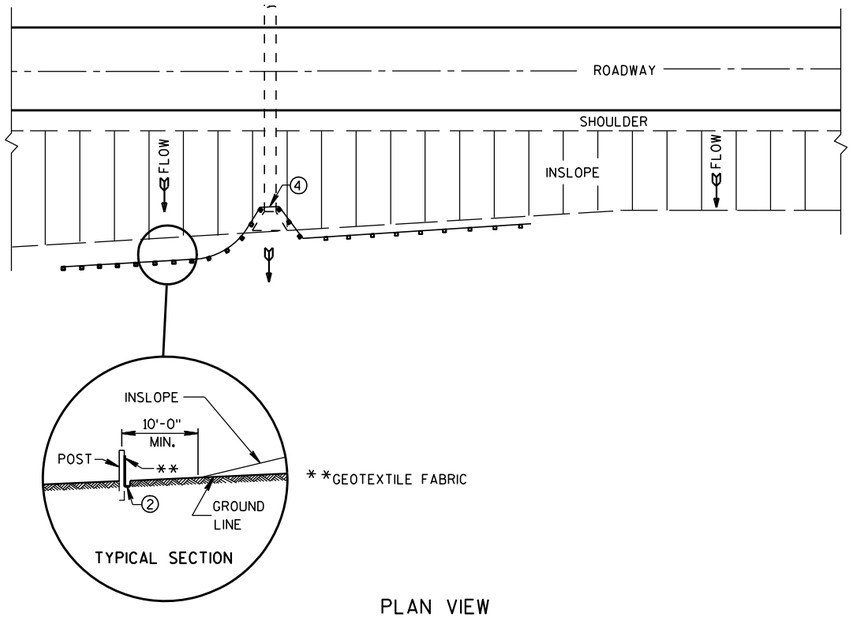
STA. 590+20.1'N' - 590+73.2'N' RT - MGS TERMINAL EAT
STA. 590+73.2'N' - 591+48.2'N' RT - MGS GUARDRAIL 3K
STA. 591+48.2'N' - 592+01.3'N' RT - MGS TERMINAL EAT
STA. 589+75'N' - 592+16'N' RT - BARRIER SYSTEM GRADING SHAPING FINISHING

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PLAN: STH 13	SHEET 57	E

Standard Detail Drawing List

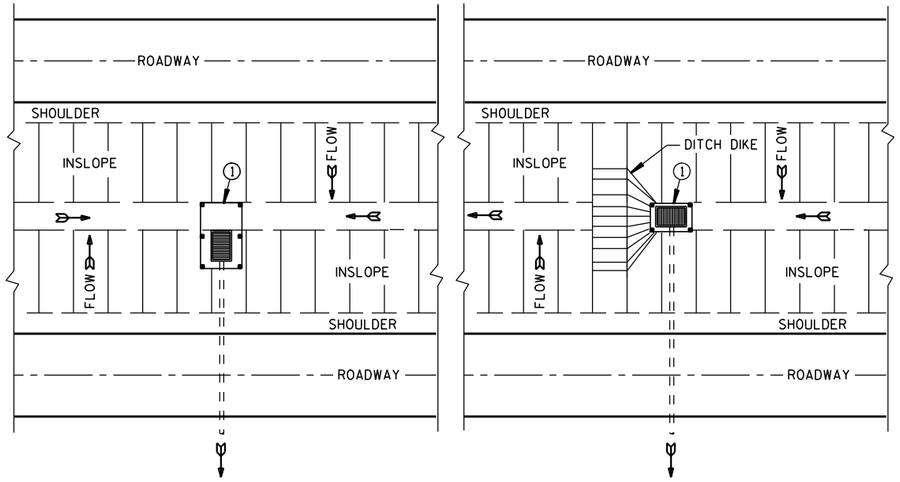
08E09-06	SILT FENCE
09A01-13A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
13A10-01A	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-01B	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-01C	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-01D	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A11-02A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-02B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
14B15-09A	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-09B	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-09C	STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS
14B18-06A	STEEL PLATE BEAM GUARD, CLASS "A" (AT BRIDGES, OBSTACLES AND SIDEROADS/DRIVEWAYS)
14B24-08A	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B24-08B	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B24-08C	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B27-01A	STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL
14B27-01B	STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL
14B27-01C	STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL
14B42-04A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-04B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-04C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B43-03A	MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L)
14B43-03B	MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L)
14B43-03C	MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L)
14B44-02A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B45-04A	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04B	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04C	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04G	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04H	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04I	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-04J	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
15A02-09	DELINEATOR POST, DELINEATOR REFLECTOR AND DELINEATOR BRACKET WITH REFLECTIVE SHEETING
15C04-03	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-03	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C08-17A	LONGITUDINAL MARKING (MAINLINE)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C19-04A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY

6



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

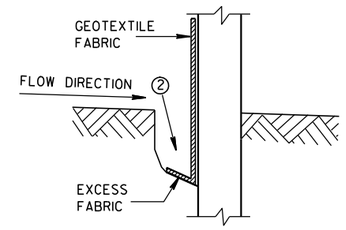
① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.

② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

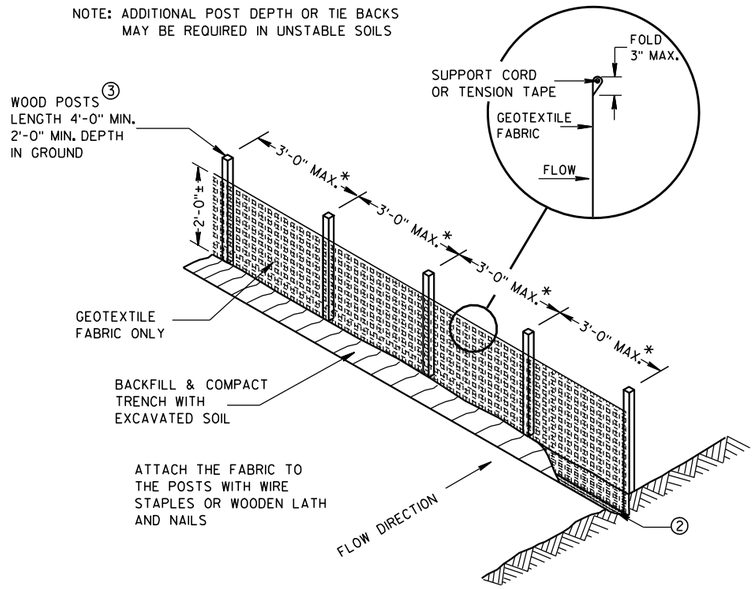
③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.

④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.

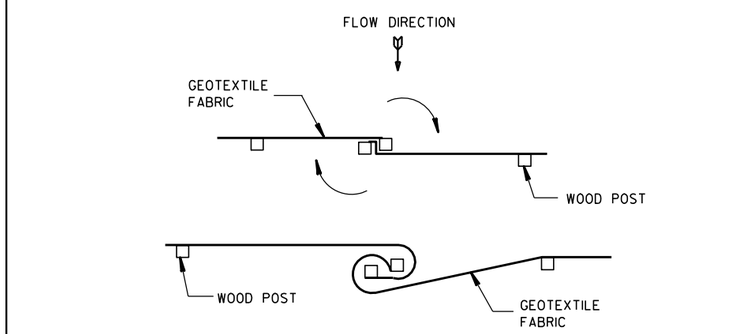
⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



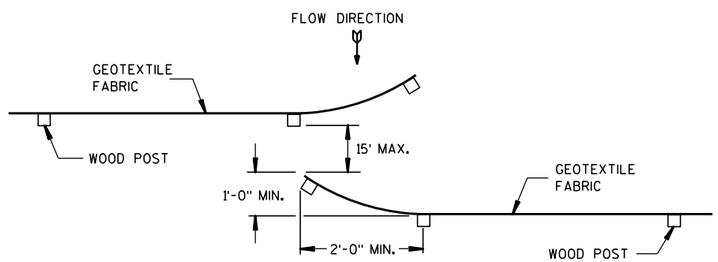
TRENCH DETAIL



SILT FENCE

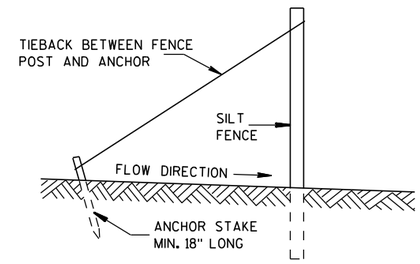


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

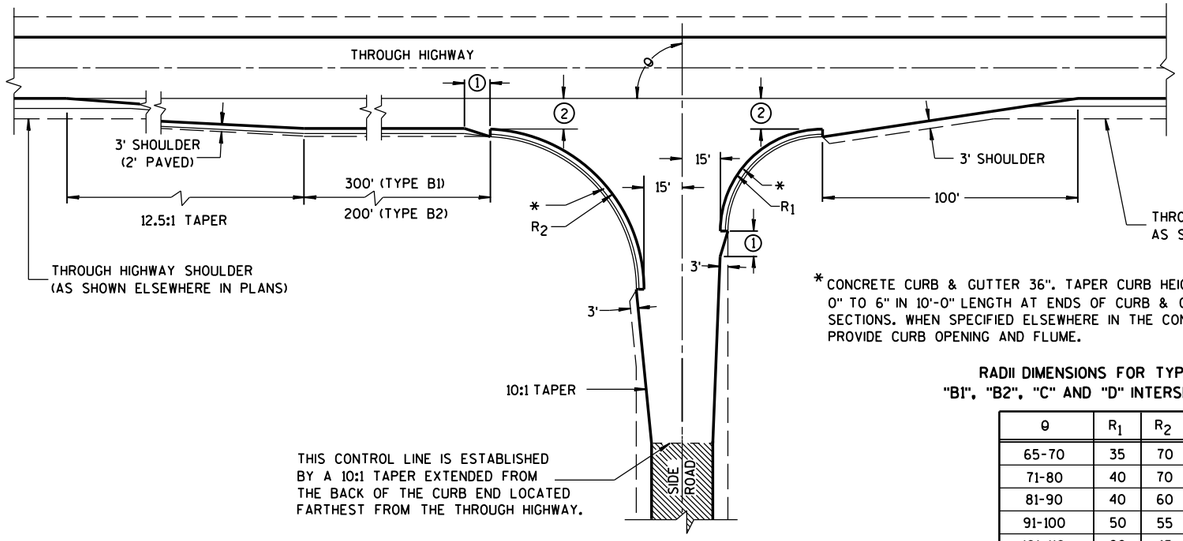
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 8 E 9-6

6

S.D.D. 8 E 9-6

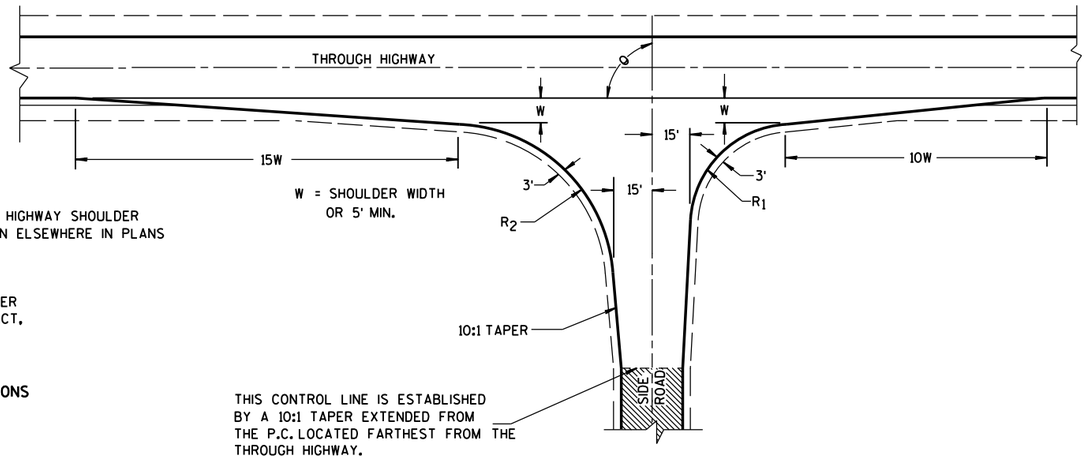


TYPE "B1" AND "B2"

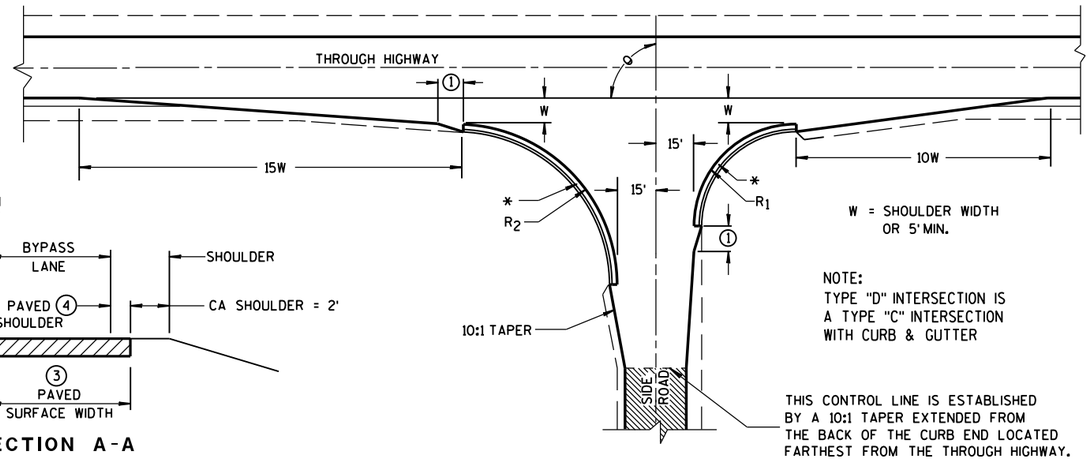
*CONCRETE CURB & GUTTER 36". TAPER CURB HEIGHT 0" TO 6" IN 10'-0" LENGTH AT ENDS OF CURB & GUTTER SECTIONS. WHEN SPECIFIED ELSEWHERE IN THE CONTRACT, PROVIDE CURB OPENING AND FLUME.

RADII DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R_1	R_2
65-70	35	70
71-80	40	70
81-90	40	60
91-100	50	55
101-110	60	45



TYPE "C"



TYPE "D"

GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

EXISTING PAVED SURFACE

BYPASS LANE

(1) 10-FT TYPICAL.

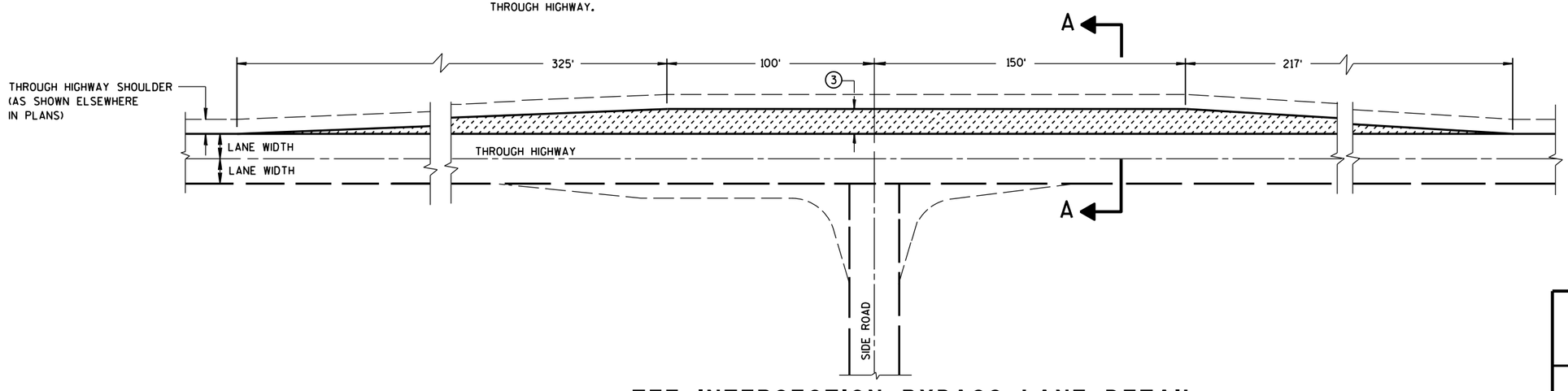
(2) 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.

**10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.

(3) BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
-ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH.
-PC CPNCRETE = 13-FT PLUS PAVED SHOULDER WIDTH.

(4) BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.

SECTION A-A
(SHOWING BYPASS LANE AND SHOULDER)

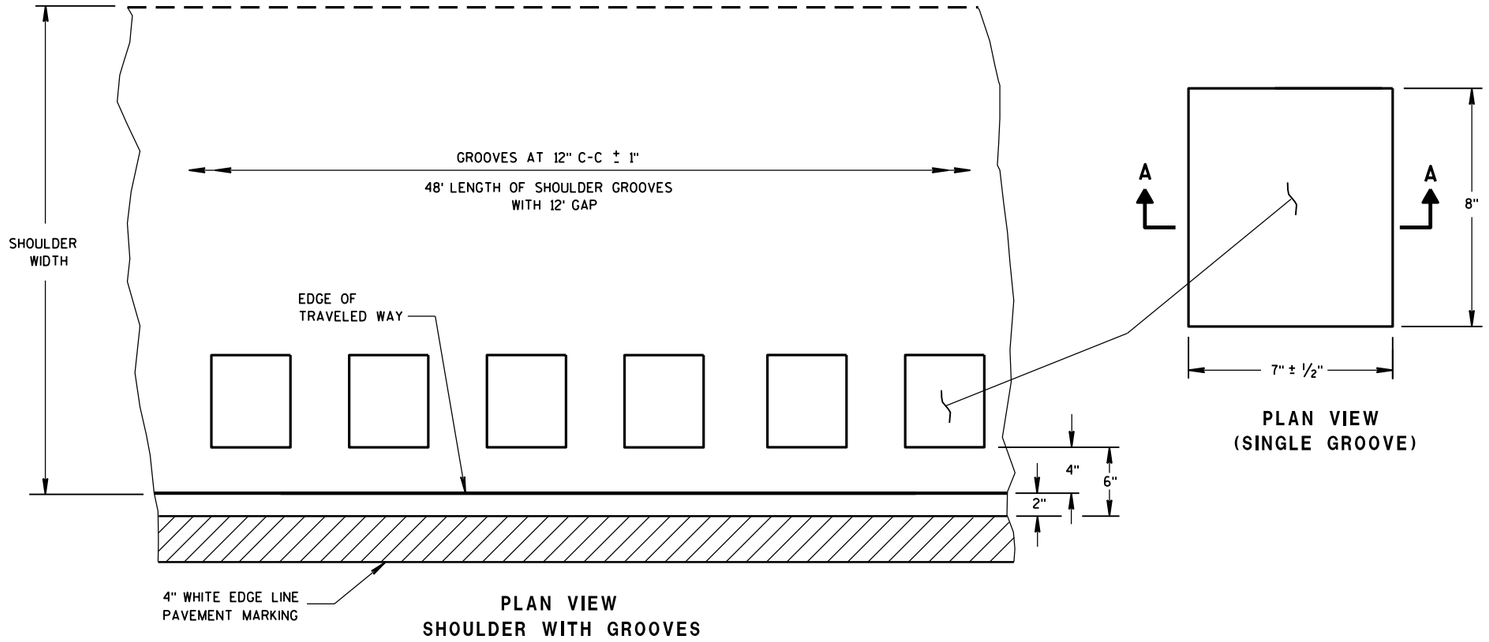


TEE INTERSECTION BYPASS LANE DETAIL

AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND "D" AND TEE INTERSECTION BYPASS LANE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6
S.D.D. 9 A 1-13a



PLAN VIEW
SHOULDER WITH GROOVES

PLAN VIEW
(SINGLE GROOVE)

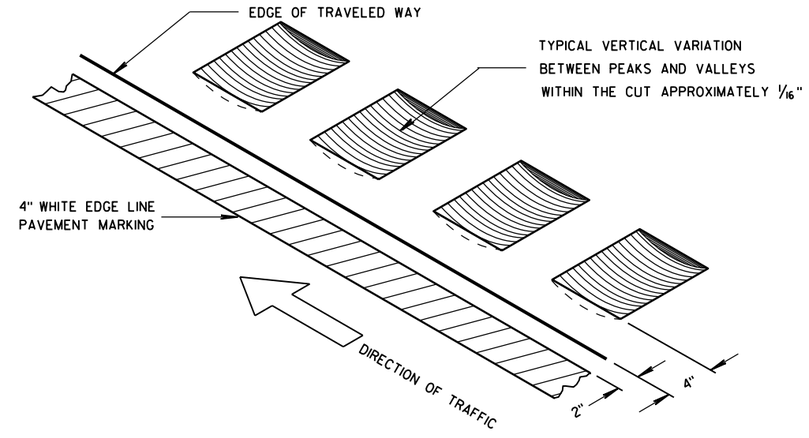
PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP

GENERAL NOTES

DETAILS OF CONSTRUCTION SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

(1) SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



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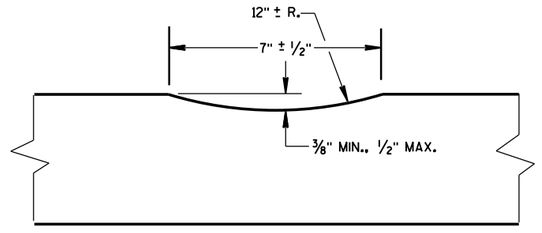
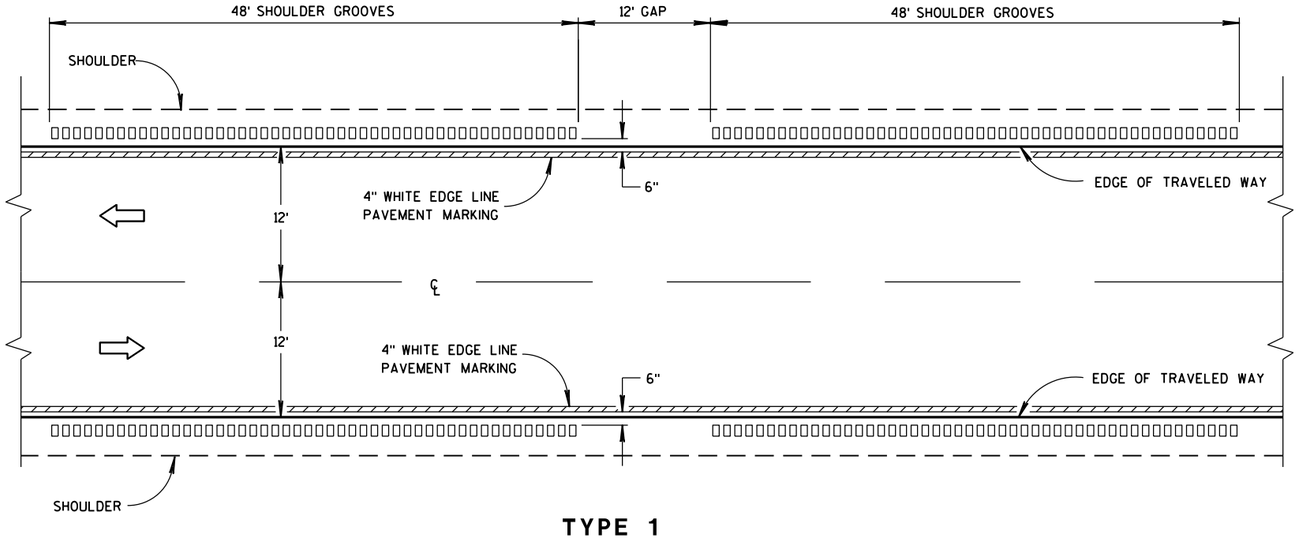


SECTION A-A



TYPE 1
2-LANE SHOULDER RUMBLE STRIP

2-LANE RURAL
SHOULDER RUMBLE STRIP, MILLING

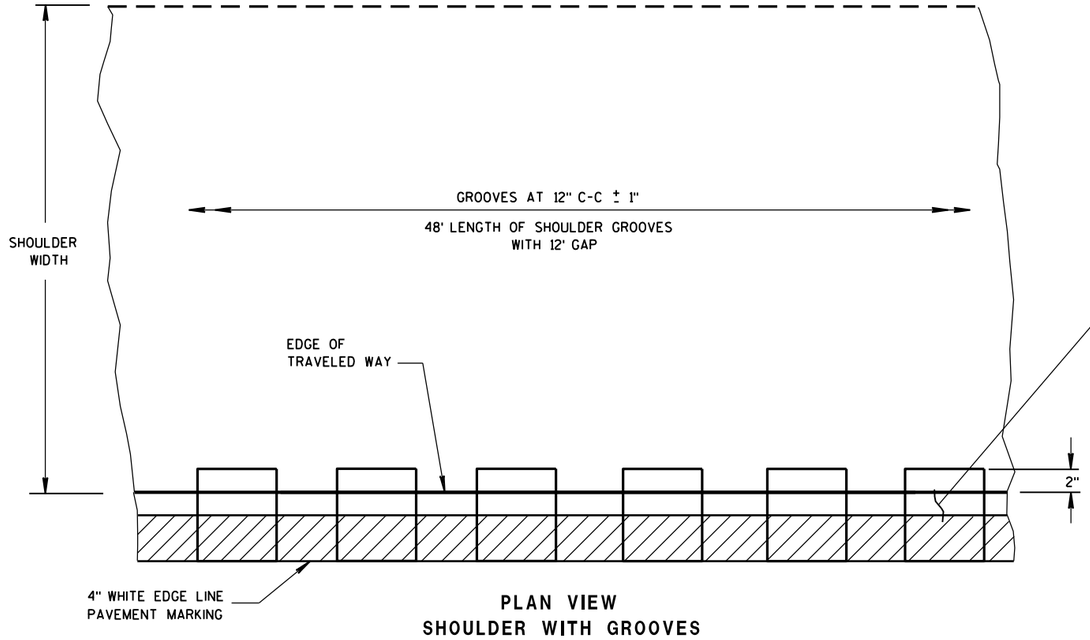
STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

6

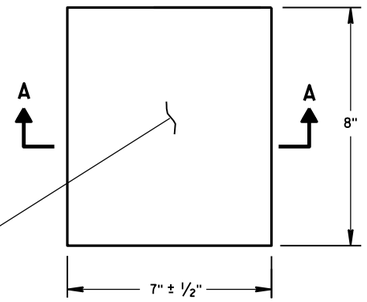
S.D.D. 13 A 10-1a

6

S.D.D. 13 A 10-1a



PLAN VIEW
SHOULDER WITH GROOVES



PLAN VIEW
(SINGLE GROOVE)

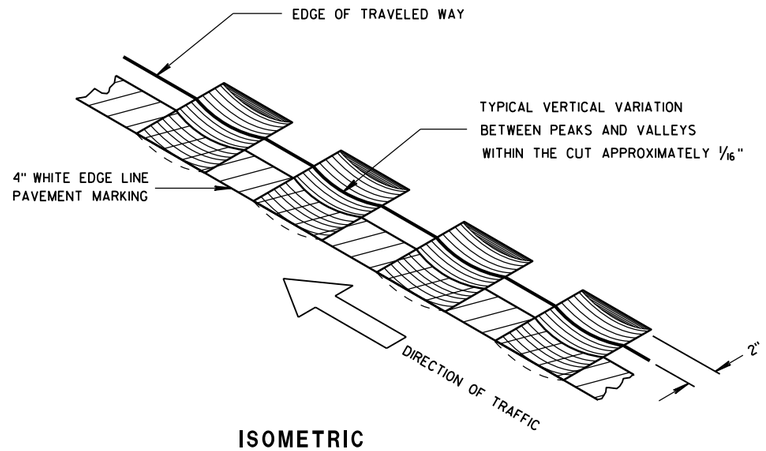
PLACEMENT DETAIL FOR TYPE 2 MILLED RUMBLE STRIP

GENERAL NOTES

DETAILS OF CONSTRUCTION SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

(1) SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



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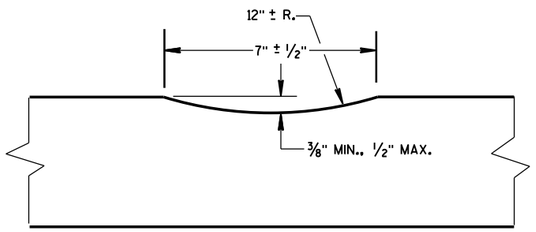
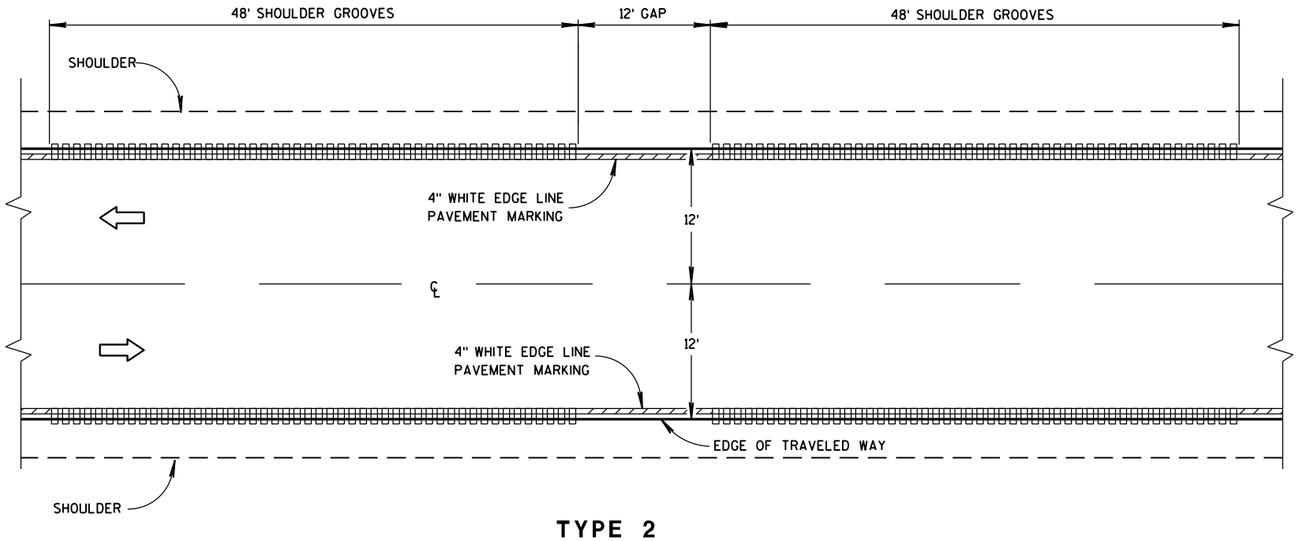


SECTION A-A



TYPE 2
2-LANE SHOULDER RUMBLE STRIP

2-LANE RURAL
SHOULDER RUMBLE STRIP, MILLING

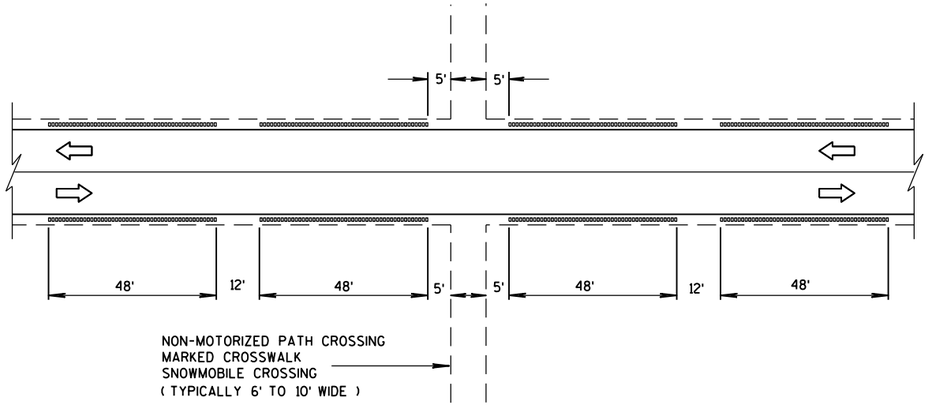
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

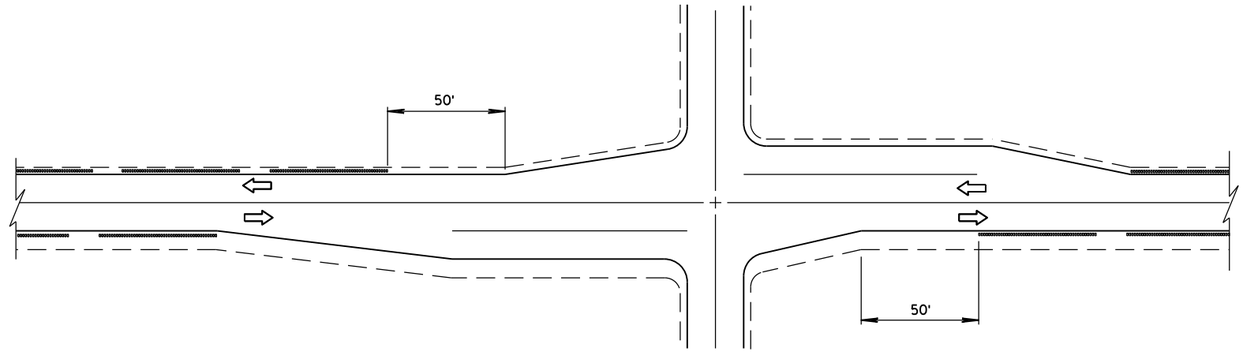
S.D.D. 13 A 10-1b

6

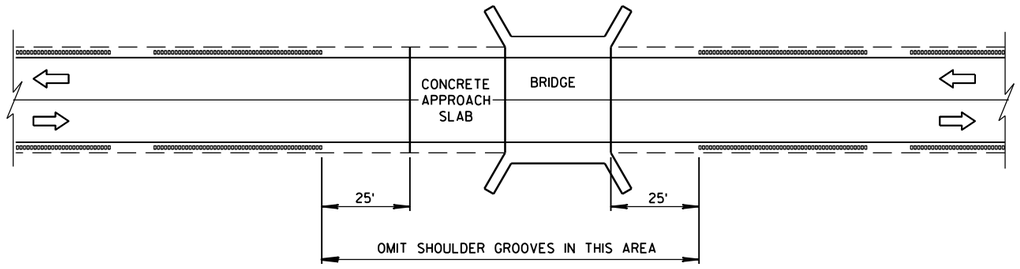
S.D.D. 13 A 10-1b



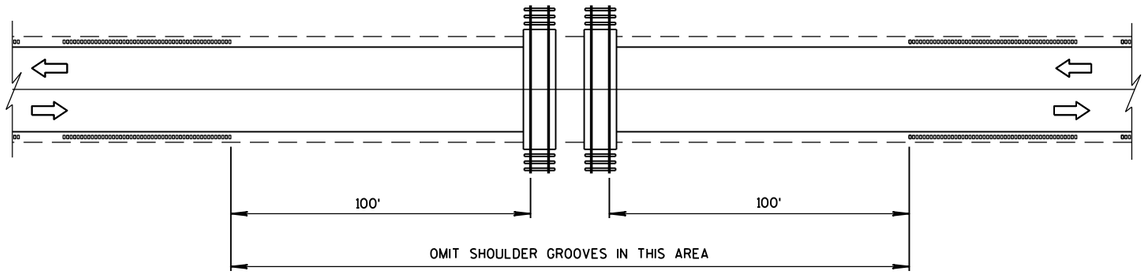
SHOULDER GROOVES AT MISCELLANEOUS CROSSINGS



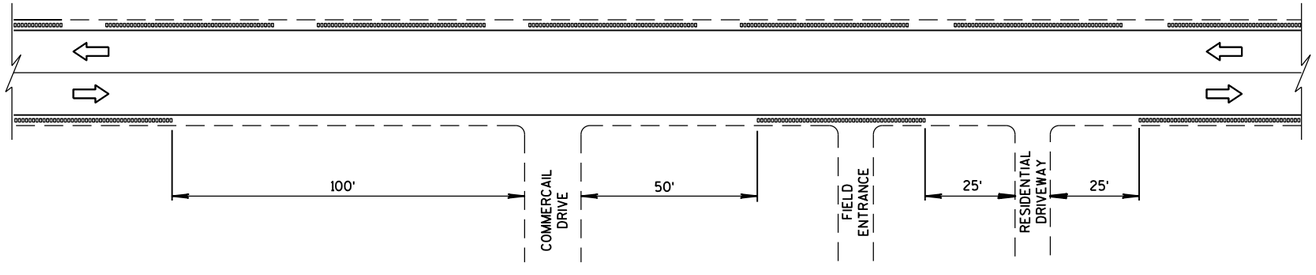
SHOULDER GROOVES AT INTERSECTIONS



SHOULDER GROOVES AT BRIDGES



SHOULDER GROOVES AT RAILROADS



SHOULDER GROOVES AT DRIVEWAYS (1)

2-LANE RURAL
SHOULDER RUMBLE STRIP, MILLING

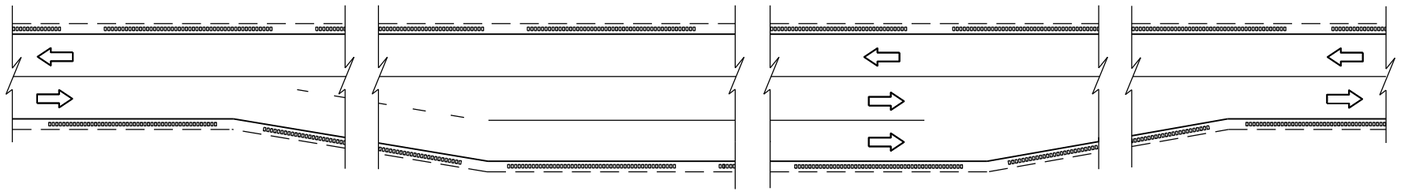
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

6

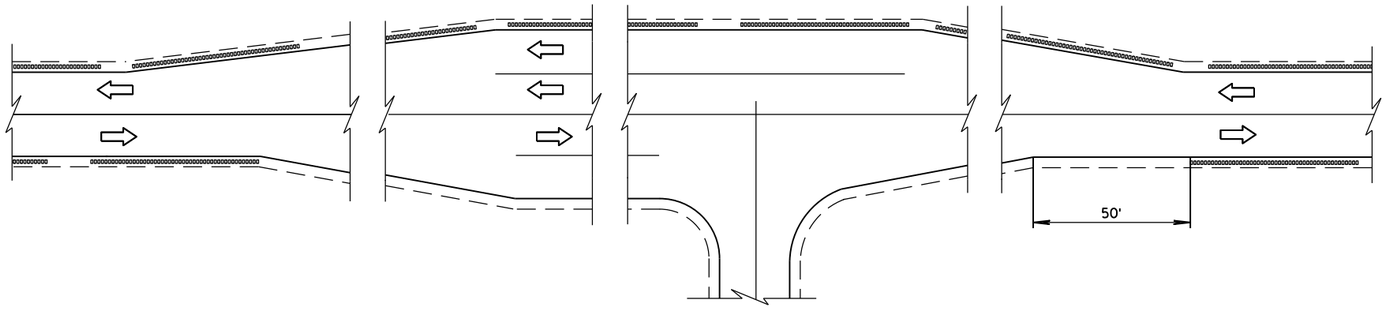
S.D.D. 13 A 10-1c

6

S.D.D. 13 A 10-1c



SHOULDER GROOVES AT PASSING AND CLIMBING LANES



SHOULDER GROOVES AT BYPASS LANES

6

6

2-LANE RURAL
SHOULDER RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

12/17/2012
DATE

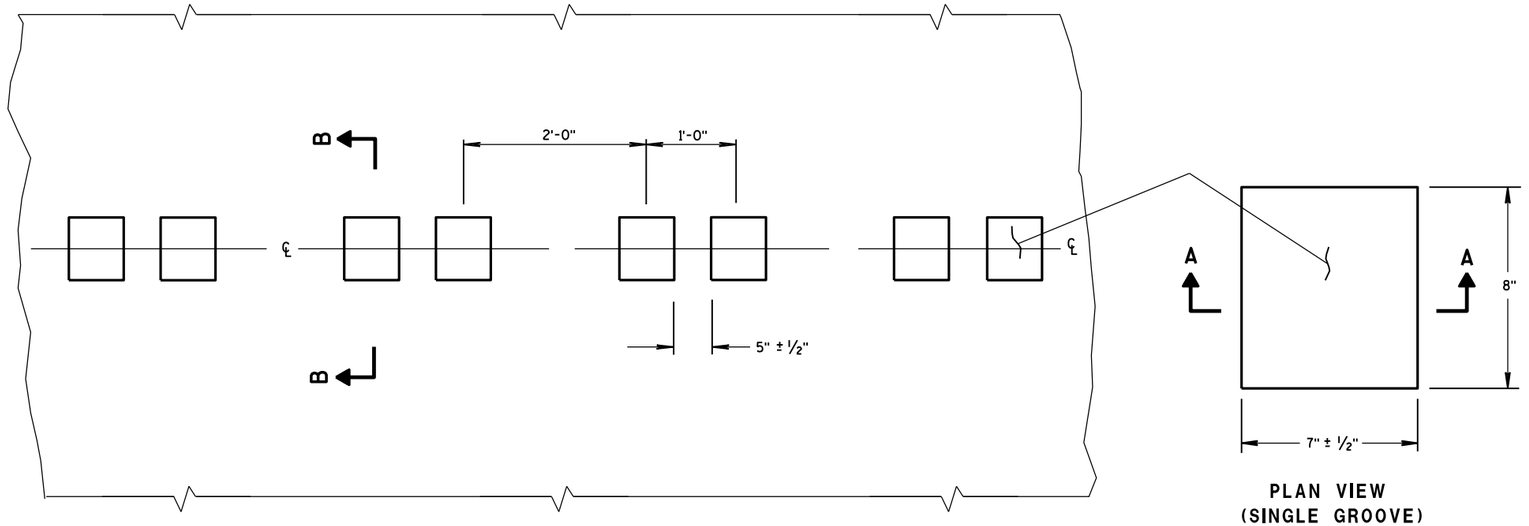
/S/ Jerry H. Zogg
ROADWAY STANDARDS ENGINEER

ENT

FHWA

S.D.D. 13 A 10-1d

S.D.D. 13 A 10-1d



PLAN VIEW
CENTER LINE WITH GROOVES

PLAN VIEW
(SINGLE GROOVE)

PLACEMENT DETAIL FOR MILLED RUMBLE STRIP

GENERAL NOTES

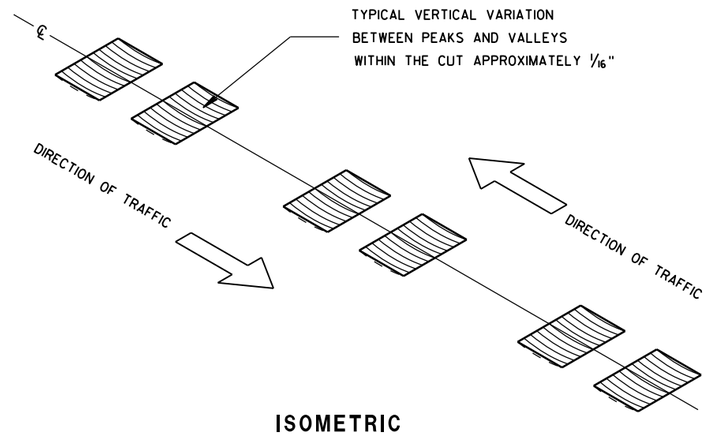
DETAILS OF CONSTRUCTION SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTER LINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

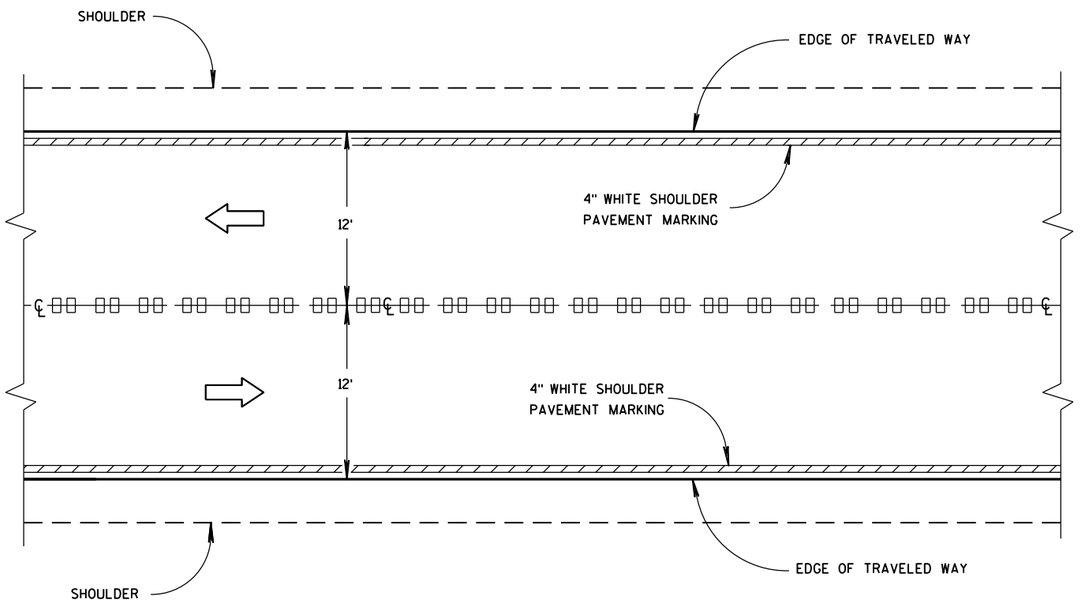
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

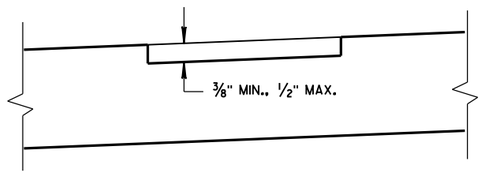
(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



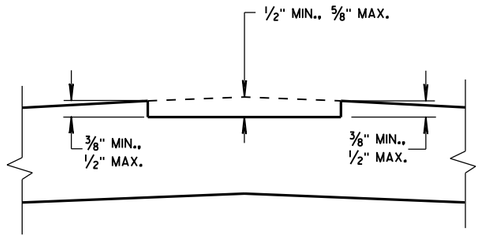
ISOMETRIC



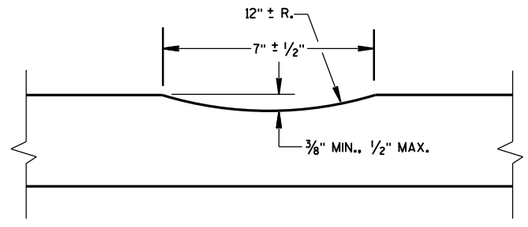
CENTER LINE GROOVES ON TWO-WAY ROADWAYS



SECTION B-B
SUPERELEVATED ROADWAY



SECTION B-B
CROWNED ROADWAY



SECTION A-A

2-LANE RURAL
CENTER LINE RUMBLE STRIP, MILLING

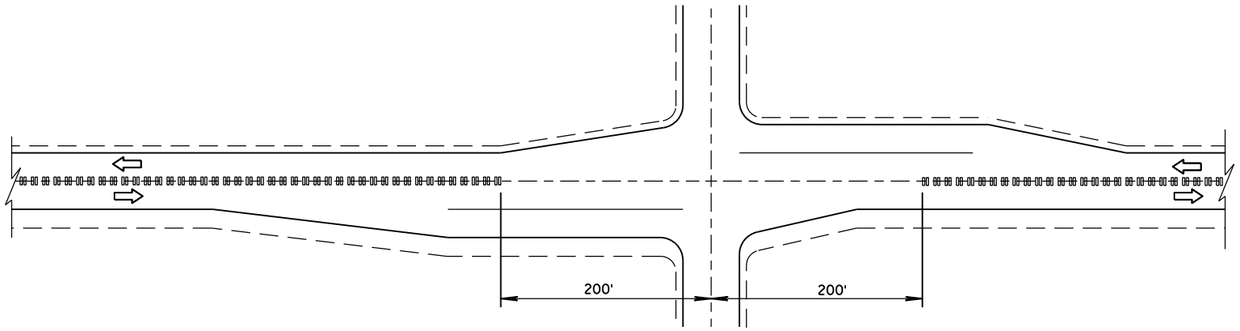
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

6

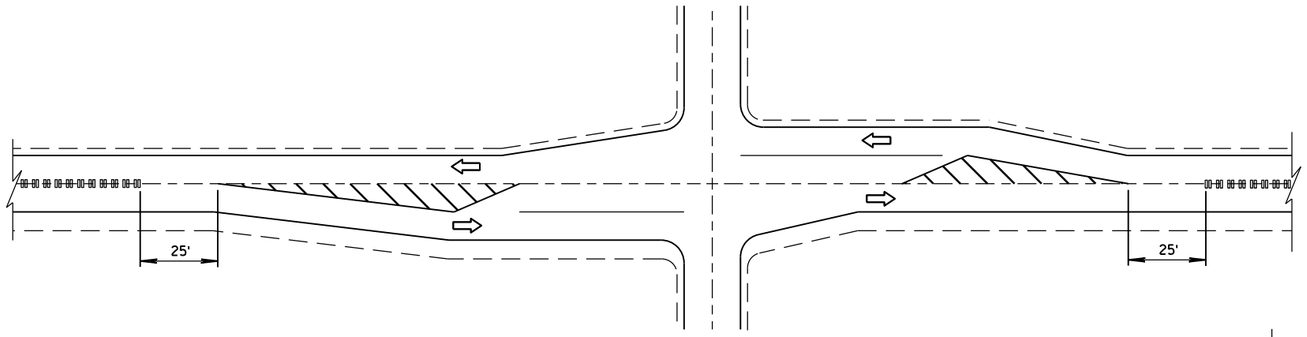
S.D.D. 13 A 11-2a

6

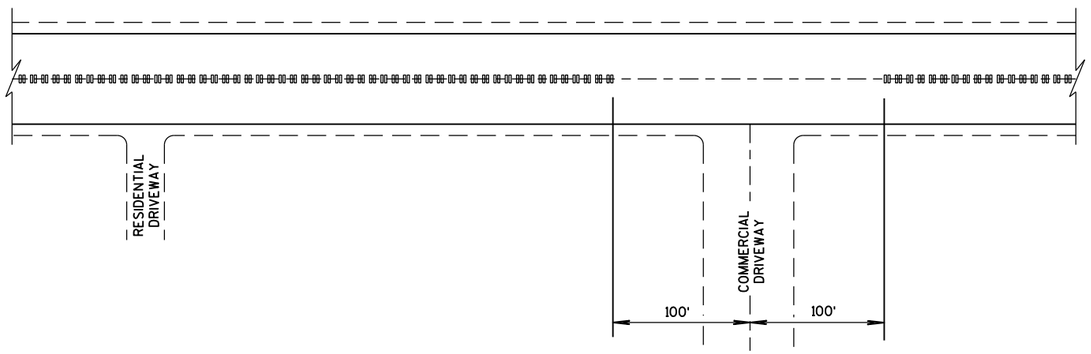
S.D.D. 13 A 11-2a



CENTER LINE GROOVES AT INTERSECTIONS

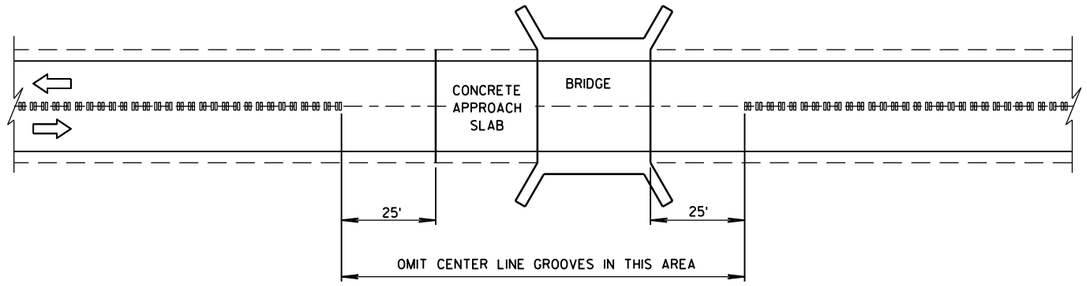


CENTER LINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)

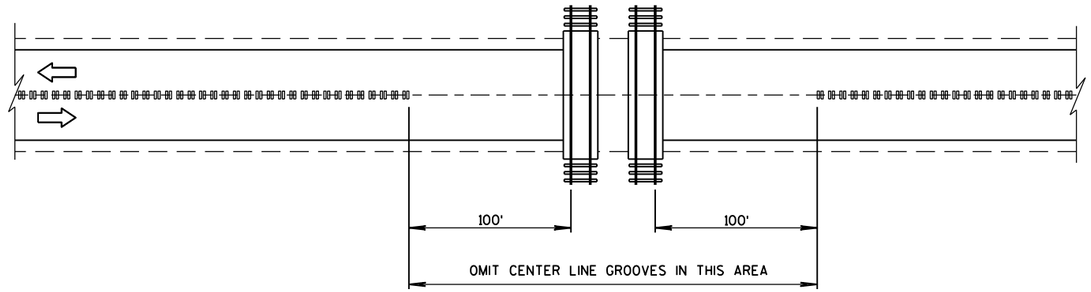


CENTER LINE GROOVES AT DRIVEWAYS (1)

(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



CENTER LINE GROOVES AT BRIDGES



CENTER LINE GROOVES AT RAILROADS

6

S.D.D. 13 A 11-2b

6

S.D.D. 13 A 11-2b

2-LANE RURAL
CENTER LINE RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5/15/2013 DATE

/S/ Jerry H. Zoda
ROADWAY STANDARDS DEVELOPMENT ENGINEER

NT

FHWA

GENERAL NOTES

(1) W6 X 9 OR W6 X 8.5 STEEL POSTS AND NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6" X 8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS. APPROVED PLASTIC BLOCKOUT DESIGNS MAY VARY FROM THIS TYPICAL DETAIL WHEN USED IN CONJUNCTION WITH STEEL POSTS.

DO NOT MIX STEEL POSTS AND WOOD POSTS IN A SINGLE INSTALLATION.

(2) USE STRUCTURAL STEEL POSTS CONFORMING TO ASTM A 36. GALVANIZED POSTS ACCORDING TO AASHTO M 111. EITHER SET THE POSTS IN DRILLED HOLES OR DRIVE TO GRADE. REMOVE MUSHROOMING CAUSED BY DRIVING AND REPAIR DAMAGED SPELTER COATING ON GALVANIZED POSTS.

(3) INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.

(4) USE EITHER WOOD OR APPROVED PLASTIC BLOCKOUTS ON WOOD POSTS.

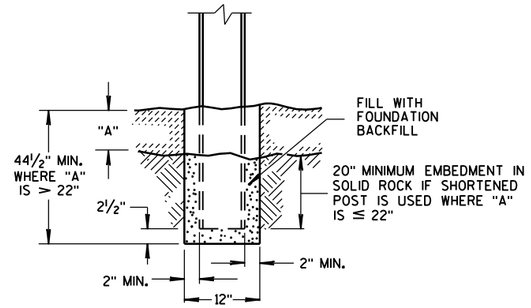
(5) IF THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING, W BEAM (LHW).

(6) IF ROCK IS ENCOUNTERED DURING EXCAVATION, THE ENGINEER MAY APPROVE USING A 12 INCH DIAMETER POST HOLE EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE APPROXIMATELY 2½ INCHES DEEP. CUT THE POSTS TO LENGTH AND PLACE IN YHE HOLE. BACKFILL WITH MATERIAL EXCAVATED FROM THE HOLE AND COMPACT ADEQUATEY.

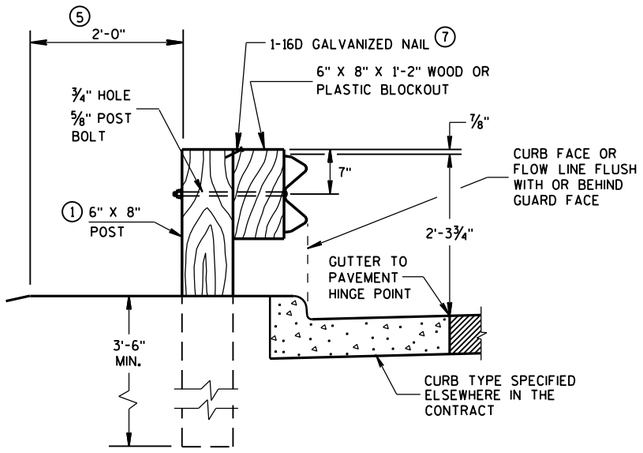
(7) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

INSTALL BEAM GUARD SECTIONS AND ALL NECESSARY HARDWARE ACCORDING TO THE APPLICABLE PLAN AND CURRENT STANDARD AND SUPPLEMENTAL SPECIFICATIONS.

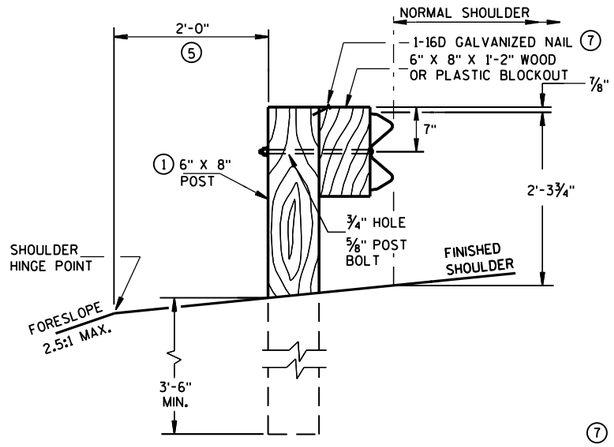
ALL DIMENSIONS ARE SUBJECT TO MANUFACTURER'S TOLERANCES EXCEPT WHERE ALLOWABLE TOLERANCES ARE SHOWN.



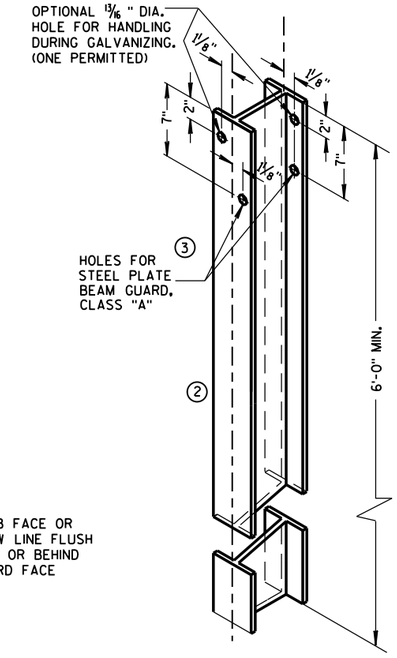
END VIEW
SETTING STEEL OR WOOD POST IN ROCK (6)



END VIEW
LOCATED ALONG A CURBED ROADWAY

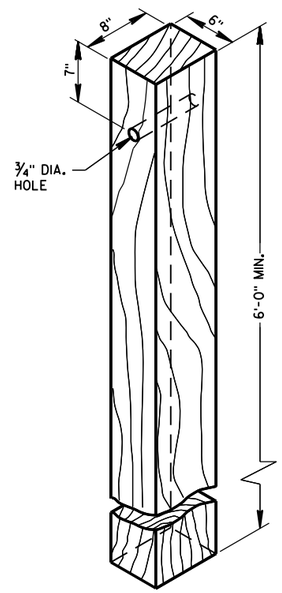


END VIEW
LOCATED ALONG A ROADWAY SHOULDER
STANDARD INSTALLATION

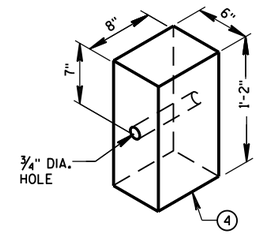


STEEL POST &
HOLE PUNCHING DETAIL
(W6 X 9) (1)

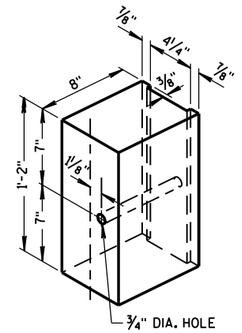
ALL HOLES 13/16" DIAMETER EXCEPT AS NOTED



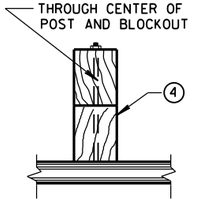
WOOD POST
(6"X8") NOMINAL



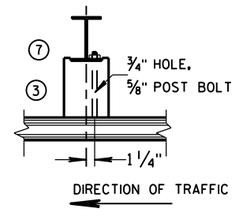
WOOD OR PLASTIC
BLOCKOUT FOR
WOOD POSTS



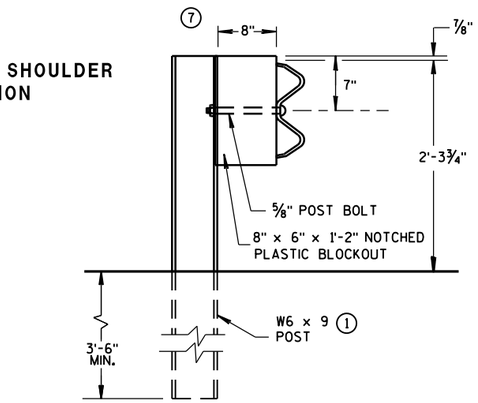
TYPICAL NOTCHED (1)
PLASTIC BLOCKOUT
FOR STEEL POSTS



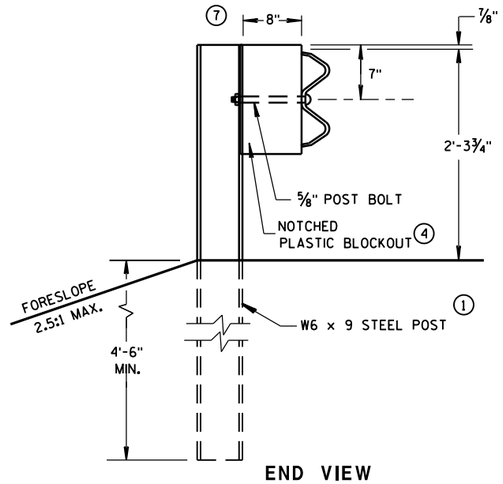
PLAN VIEW
WOOD POST, BLOCKOUT & BEAM



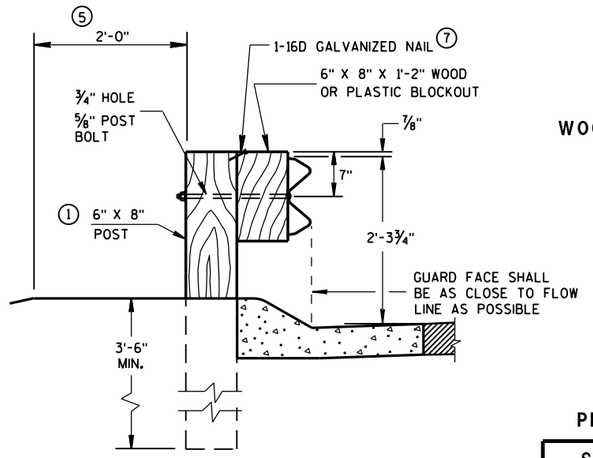
PLAN VIEW
STEEL POST, NOTCHED
PLASTIC BLOCKOUT & BEAM



END VIEW
STEEL POST & NOTCHED
PLASTIC BLOCKOUT ALTERNATIVE
STANDARD INSTALLATION



END VIEW
LONGER POST AT HALF
POST SPACING W BEAM
(LHW)



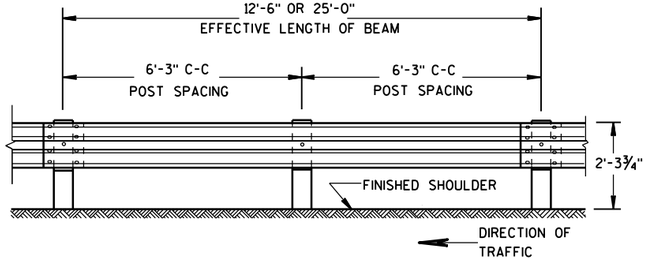
END VIEW
LOCATED ALONG A
MOUNTABLE CURBED ROADWAY

TYPICAL INSTALLATION OF STEEL PLATE BEAM GUARD

STEEL PLATE BEAM GUARD,
CLASS "A"
INSTALLATION & ELEMENTS

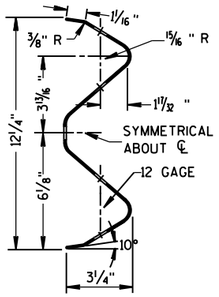
STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

6
S.D.D. 14 B 15-9a

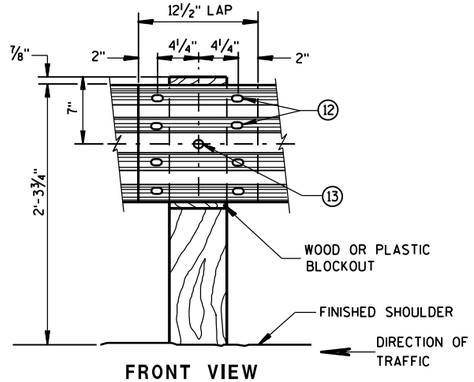


FRONT VIEW

POST SPACING STANDARD INSTALLATION



SECTION THRU W BEAM



FRONT VIEW

BEAM SPLICE AT WOOD POST AND POST MOUNTING DETAIL

GENERAL NOTES

(8) PROVIDE SILVER REFLECTIVE SHEETING ON ALL REFLECTORS EXCEPT THOSE LOCATED ALONG THE LEFT EDGE OF ONE-WAY ROADWAYS, WHICH SHALL BE PROVIDED WITH YELLOW REFLECTIVE SHEETING. SHEETING IS TYPE H. SEE STANDARD SPECIFICATION 637.

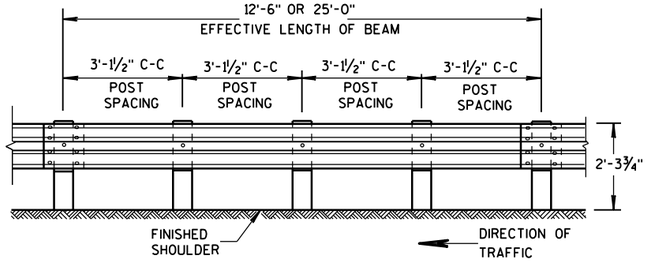
(9) DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

(10) REVERSE EVERY OTHER REFLECTOR FOR 2-WAY VISIBILITY. THE CONTRACTOR MAY FURNISH TWO-SIDED REFLECTORS IN LIEU OF ONE-SIDED REFLECTORS.

(11) PROVIDE AN ANGLE OF BEND OF 90° ± 1° FOR TWO-SIDED REFLECTORS.

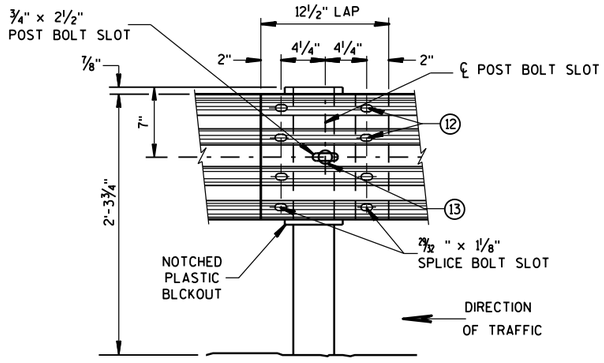
(12) 8 -⅝" ϕ X 2" BUTTON HEAD BOLTS WITH OVAL SHOULDERS & RECESS NUTS.

(13) ⅝" DIA. BUTTON HEAD BOLT AND RECESS NUT WITH ⅝" DIA. F844 FLAT WASHER UNDER NUT.



FRONT VIEW

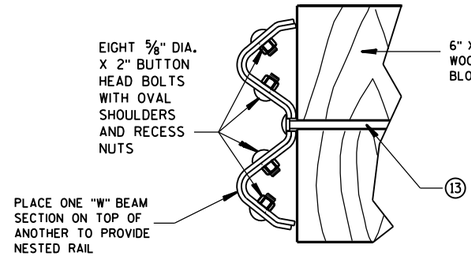
POST SPACING FOR LONGER POST AT HALF POST SPACING W BEAM (LHW)



FRONT VIEW

BEAM SPLICE AT STEEL POST

TYPICAL SPLICING DETAILS OF STEEL PLATE BEAM GUARD

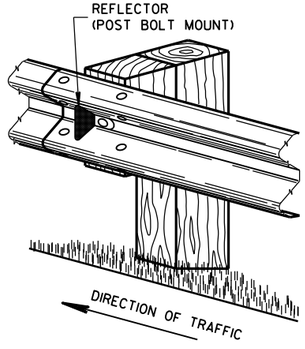


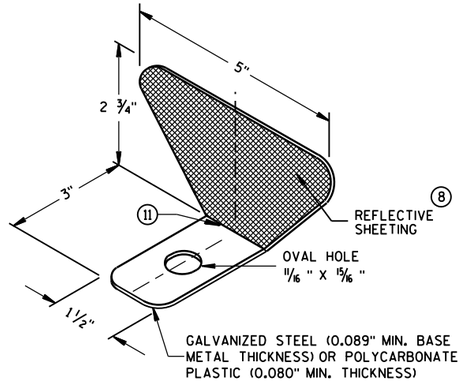
NESTED W BEAM (NW)

USE ALL OTHER STANDARD BEAM GUARD DETAILS FOR CONSTRUCTING NESTED W BEAM (NW)

REFLECTOR SPACING (9)

	BEAM GUARD LENGTH	REFLECTOR SPACING	NO. SURFACES REFLECTORIZED	MIN. NO. REFLECTORS
ONE WAY TRAFFIC	< 200'	50' C-C	1	3
	> 200'	100' C-C	1	
TWO WAY TRAFFIC	< 200'	25' C-C	1 (10)	6
	> 200'	50' C-C	1	
TWO WAY TRAFFIC	< 200'	50' C-C	2 (11)	3
	> 200'	100' C-C	2	





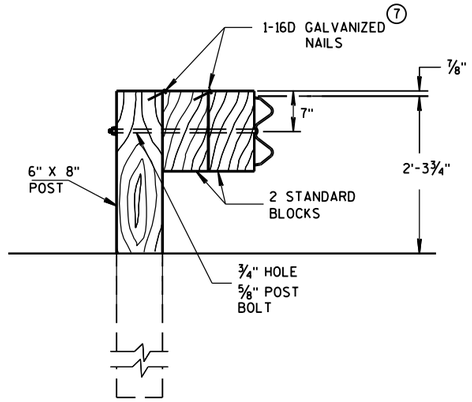
ONE SIDED REFLECTOR DETAIL AND TYPICAL INSTALLATION (8)

STEEL PLATE BEAM GUARD,
CLASS "A",
INSTALLATION & ELEMENTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

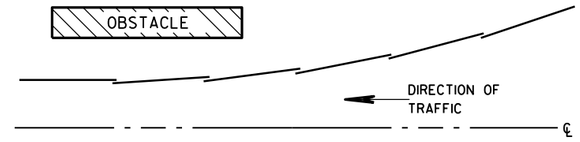
6

S.D.D. 14 B 15-9b

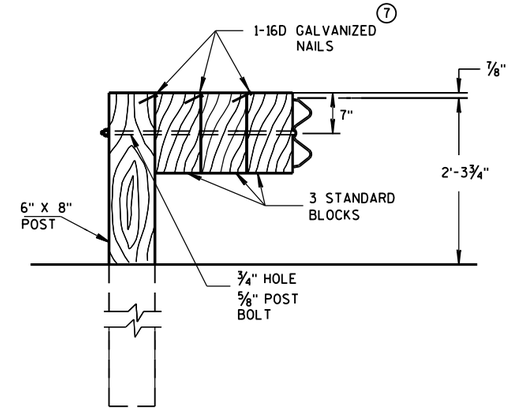


DETAIL FOR DOUBLE BLOCKS

THE NUMBER OF DOUBLE BLOCK POSTS WITHIN A BARRIER RUN IS UNLIMITED

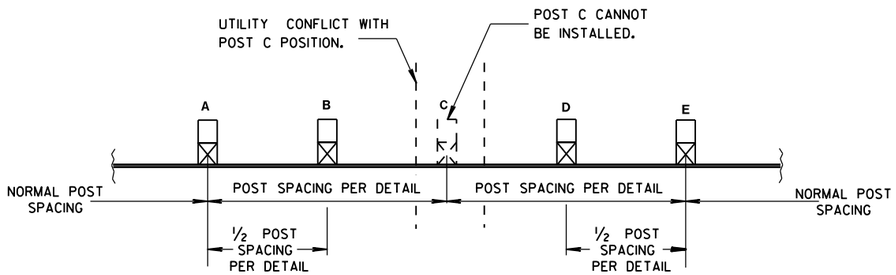


PLAN VIEW
BEAM LAPPING DETAIL



DETAIL FOR TRIPLE BLOCKS

TRIPLE BLOCK DETAIL IS LIMITED TO ONE LOCATION WITHIN A BEAM GUARD RUN.



POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION

NOTES: USE DOUBLE OR TRIPLE BLOCKS WHEN UNDERGROUND OBSTACLES PREVENT THE POST FROM BEING INSTALLED.

DO NOT USE EXTRA BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2016
DATE

/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER

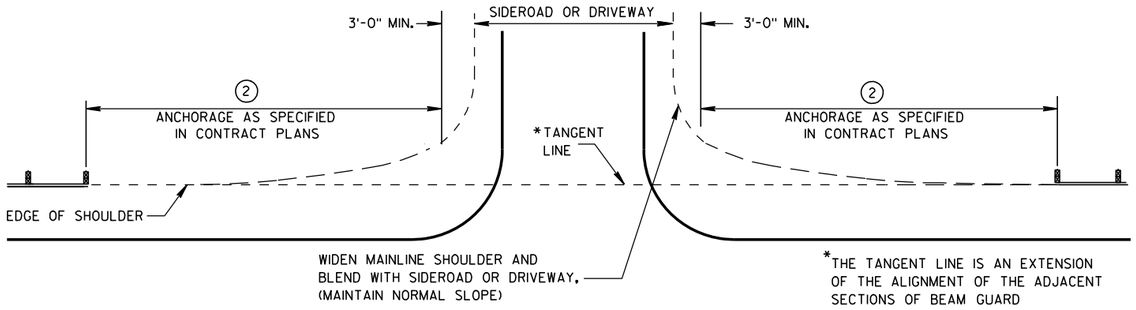
FHWA

6

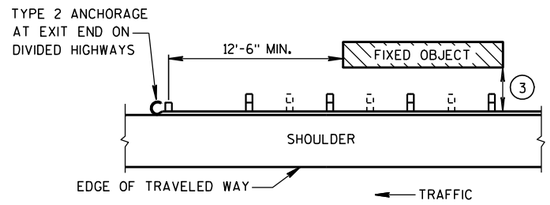
S.D.D. 14 B 15-9c

6

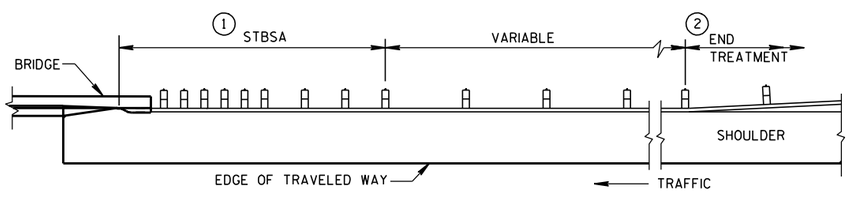
S.D.D. 14 B 15-9c



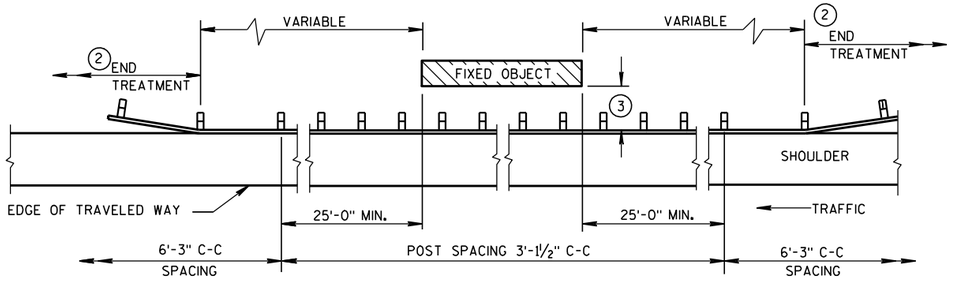
BEAM GUARD AT SIDEROADS OR DRIVEWAYS



BEAM GUARD AT OBSTACLES EXIT END - ONE WAY TRAFFIC

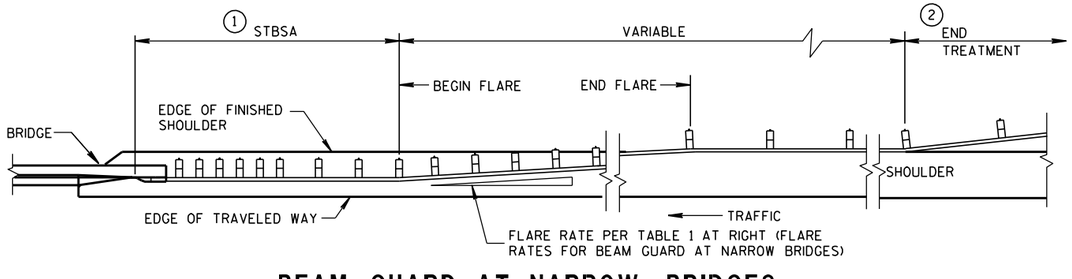


BEAM GUARD AT FULL WIDTH BRIDGES



BEAM GUARD AT OBSTACLES - TWO WAY TRAFFIC

(RAIL TO OBSTACLE CLEARANCE 3'-6" TO 4'-6")



BEAM GUARD AT NARROW BRIDGES

(FLARED TO SHOULDER EDGE, THEN PARALLEL TO ROADWAY)

TABLE 1
FLARE RATES FOR BEAM GUARD AT NARROW BRIDGES

POSTED SPEED (MPH)	FLARE RATE
25	13:1
30	15:1
35	16:1
40	18:1
45	21:1
50	24:1
55	26:1
65	30:1

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE PERTINENT STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

W6 X 9 OR W6 X 8.5 STEEL POSTS WITH NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6" X 8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS.

THE LOCATIONS AND LENGTHS OF BEAM GUARD ARE SHOWN ELSEWHERE IN THE PLAN.

(1) STEEL THRIE BEAM STRUCTURAL APPROACH (STBSA) - SEE CURRENT SDD 14B20.

(2) USE AN APPROVED END TREATMENT FOR THE TRAFFIC APPROACH SIDE OF BRIDGE/OBSTACLES. USE TYPE 2 ANCHORAGE ONLY AT THE DOWNSTREAM ENDS OF BEAM GUARD LOCATED ALONG ROADWAYS WITH ONE WAY TRAFFIC.

(3)

MINIMUM LATERAL DISTANCE FROM FACE OF BEAM GUARD TO FIXED OBJECT	POST SPACING
3'-6"	3' - 1½"
4'-6"	6' - 3"

STEEL PLATE BEAM GUARD CLASS "A" AT BRIDGES, OBSTACLES AND SIDEROADS/DRIVEWAYS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8-21-07 DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 14 B 18-6a

BILL OF MATERIALS

NOTE NO.	QTY.	DESCRIPTION
①	4	WOOD BREAKAWAY TERMINAL POST: 5½" X 7½" X 3'-9"
②	**	STEEL TUBE: OPTION 1 - QUANITY OF 4 TS 8" X 6" X 0.188", 4'-6" LONG OR OPTION 2 - QUANITY OF 2 TS 8" X 6" X 0.188", 6'-0" AND 2 TS 8" X 6" X 0.188", 4'-6" LONG
③	2	SOIL PLATE: 2'-0" X 1'-6" X ¼" **
④	4	WOOD BREAKAWAY CRT POST: 6" X 8" X 6'-0"
⑤	6	WOOD OFFSET BLOCKS: 6' X 8" X 1'-2"
⑥	1	PIPE SLEEVE: 2" X 5 ½" STANDARD PIPE
⑦	1	BEARING PLATE
⑧	1	BCT CABLE ASSEMBLY
⑨	1	CABLE ANCHOR BOX
⑩	1	STRUT & YOKE
⑪	1	STEEL PLATE BEAM, END PANEL 12 GA. 13'-6½" LONG FOR SKT-350, ET-2000 AND ET-2000 PLUS
⑫	3	STEEL PLATE BEAM: 12 GA. 13'-6½"
⑬	1	ET-2000/ET-2000 PLUS GUARDRAIL EXTRUDER OR SKT-350 IMPACT HEAD: AS FURNISHED BY MANUFACTURER
⑭	1	0.040" ALUMINUM SHEET WITH REFLECTIVE SHEETING TYPE F PER SECTION 637 OF THE STANDARD SPECIFICATIONS
⑮	1	E.A.T. MARKER POST

GENERAL NOTES

FOLLOW MANUFACTURE'S BOLTING RECOMMENDATIONS. IF NONE ARE AVAILABLE, INSTALL ⅝" ø X 1'-6" BUTTON HEAD BOLTS AT ALL POSTS EXCEPT FOR POST 1.

(A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL), AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.

(B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED.

(C) THE 13 SLOT FIRST RAIL PANEL MAY BE USED IN LIEU OF THE 3 SLOT RAIL PANEL ON SKT-350 ONLY.

(D) THE TOP OF THE STEEL TUBE ON POSTS 1 THROUGH 4 SHALL NOT BE MORE THAN 3" ABOVE THE FINISH GROUND ELEVATION.

(E) THE CENTER OF THE UPPER 3½" DIAMETER HOLE ON POST 5 THROUGH 8 SHALL BE ¾" ABOVE THE FINISHED GROUND LINE.

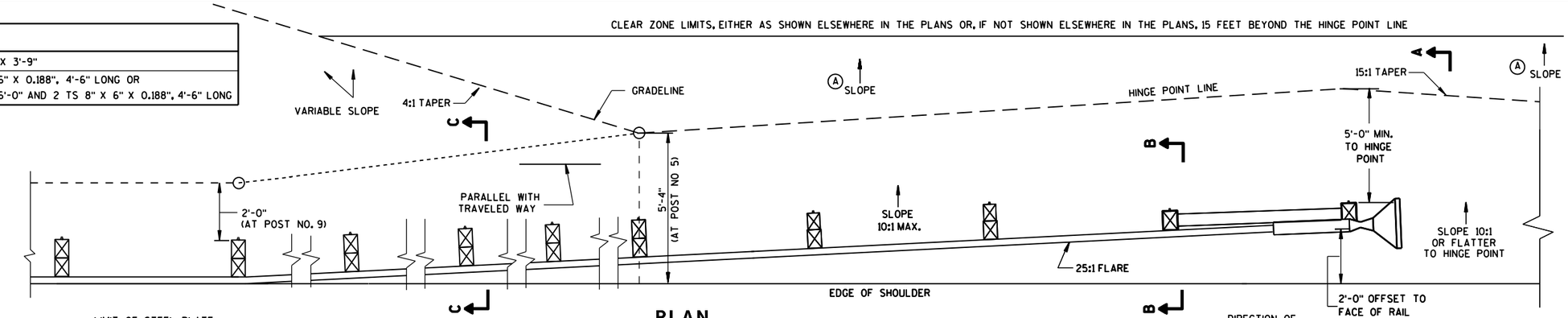
(F) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF-TAPPING SCREWS. ONE SCREW PER CORNER.

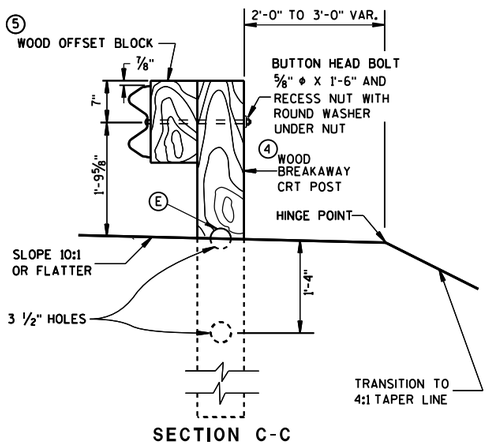
STEEL POSTS SHALL NOT BE ALLOWED FOR USE WITH ENERGY ABSORBING TERMINALS.

DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

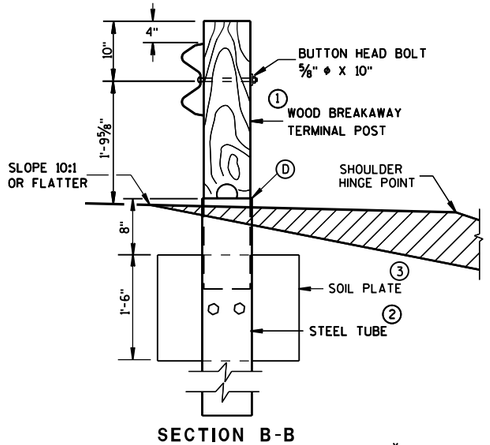
* DO NOT ATTACH BLOCKOUTS TO POSTS 1 AND 2.

** SDD SHOWS 4 - 54 INCH STEEL TUBES WITH SOIL PLATES INSTALLED ON POST 1 AND POST 2. POST 3 AND 4 DO NOT NEED SOIL PLATES. AN ALTERNATIVE INSTALLATION WOULD CONSIST OF 2 - 72 INCH STEEL TUBES ON POST 1 AND POST 2 AND 54 INCH SOIL TUBES ON POSTS 3 AND 4. THE ALTERNATIVE INSTALLATION DOES NOT REQUIRE SOIL PLATES.

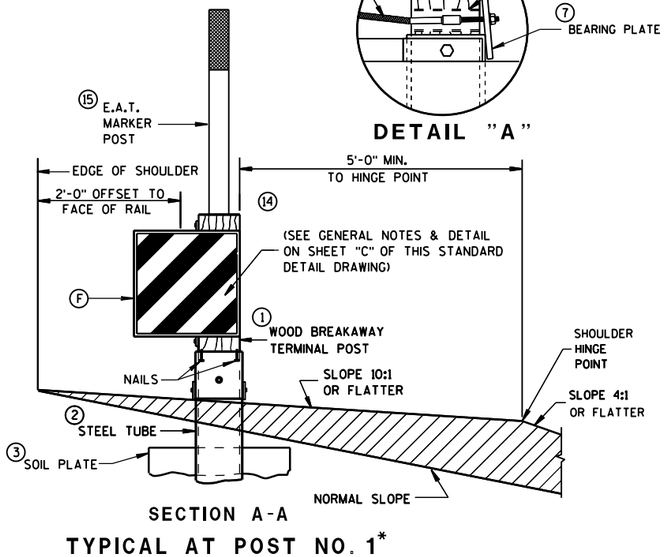




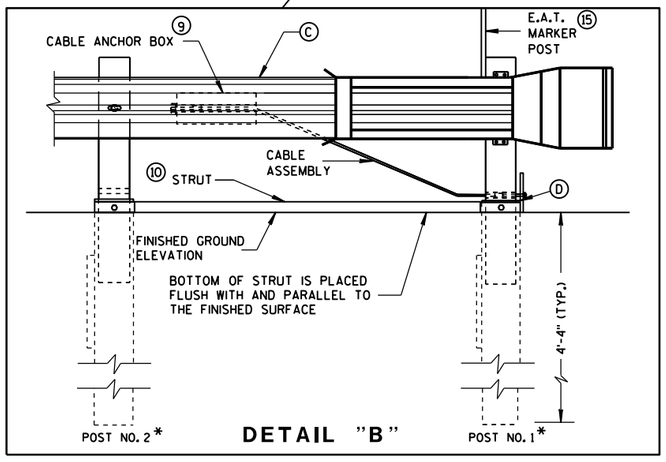
SECTION C-C
TYPICAL AT POST NOS. 6, 8



SECTION B-B
TYPICAL AT POST NO. 2*



SECTION A-A
TYPICAL AT POST NO. 1*



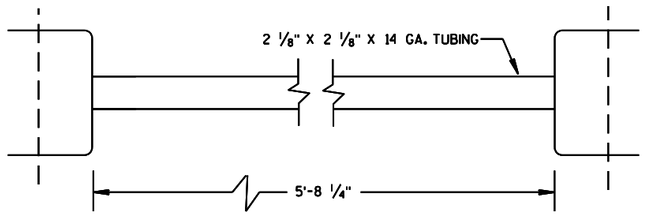
DETAIL "B"

STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL

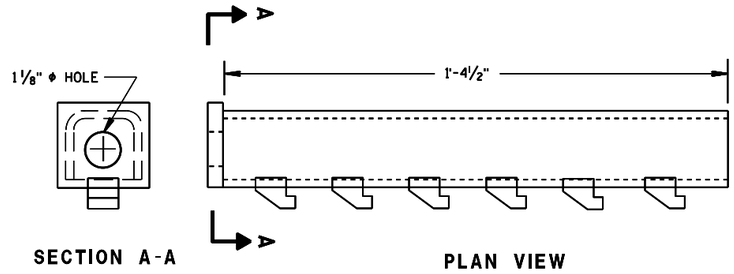
STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

6

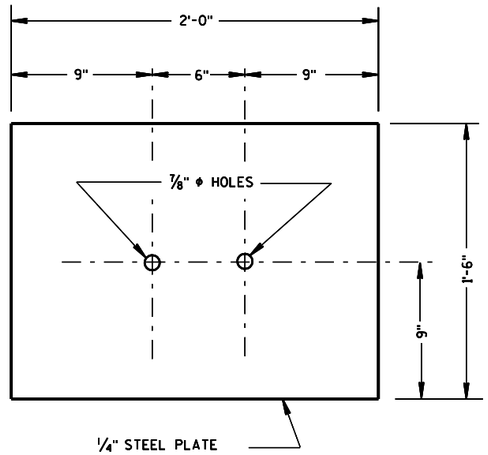
S.D.D. 14 B 24-8a



⑩ STRUT DETAIL (SKT-350)

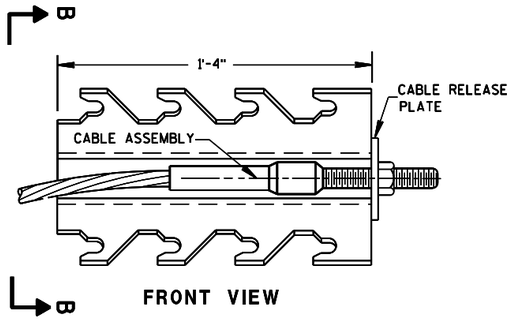


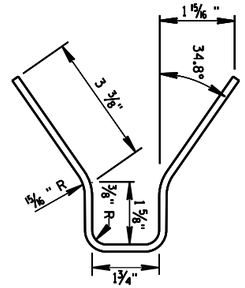
⑨ CABLE ANCHOR BOX (ET-2000/ET-2000 PLUS)



③ SOIL PLATE

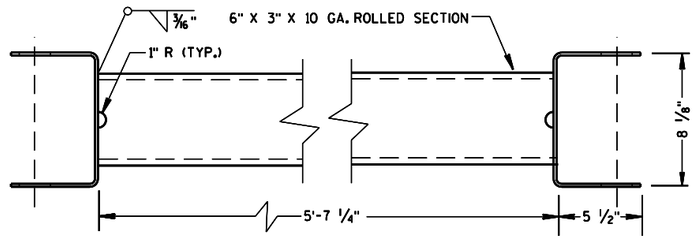
(SKT-350, ET-2000/ET-2000 PLUS)





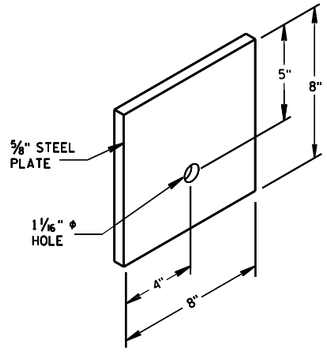
⑨ CABLE ANCHOR BOX (SKT-350)

(SKT-350)



⑩ STRUT DETAIL (ET-2000/ET-2000 PLUS)

(ET-2000/ET-2000 PLUS)



⑦ STEEL BEARING PLATE

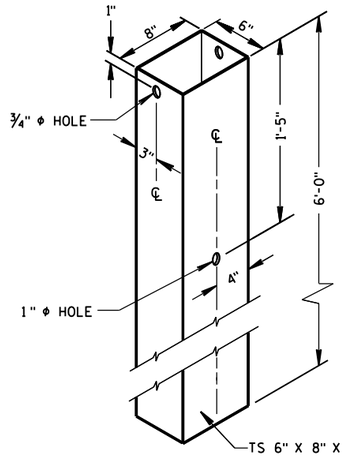
(SKT-350, ET-2000/ET-2000 PLUS)

STEEL PLATE BEAM GUARD
ENERGY ABSORBING TERMINAL

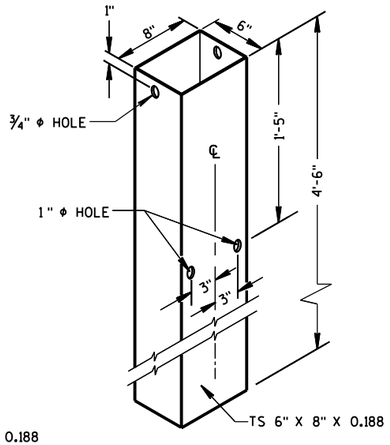
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

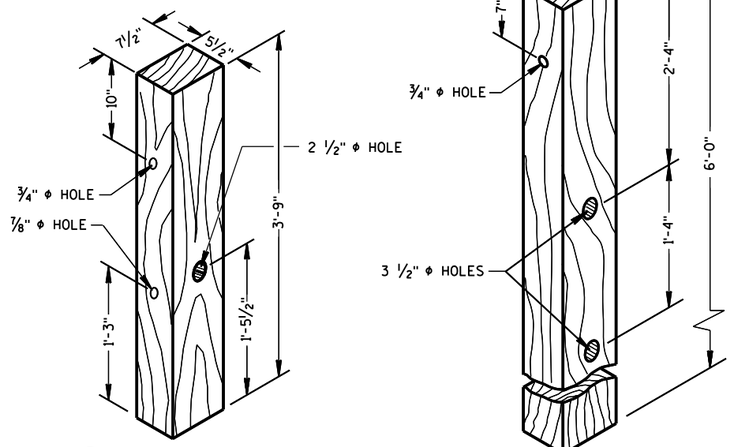
S.D.D. 14 B 24-8b



② 72" STEEL TUBE
(POSTS NO. 1-4



② 54" STEEL TUBE
(POSTS NO. 1-4



① TERMINAL POST
(POSTS NO. 1-4

④ CRT POST
(POSTS NO'S 5-8

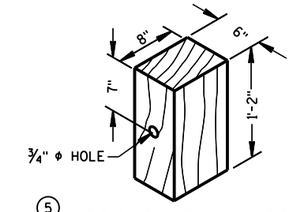
WOOD BREAKAWAY POSTS

GENERAL NOTES

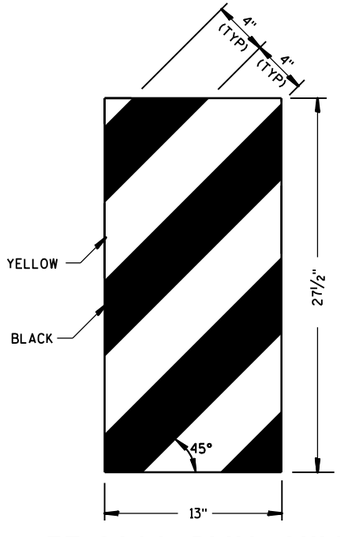
WHEN ROCK IS ENCOUNTERED DURING EXCAVATION, A 12 INCH DIA. POST HOLE EXTENDING 20 INCHES DEEP INTO THE ROCK MAY BE USED IF APPROVED BY THE ENGINEER. GRANULAR MATERIAL SHALL BE PLACED IN THE BOTTOM OF THE HOLE APPROXIMATELY 2 ½" INCHES DEEP TO PROVIDE DRAINAGE. THE SOIL TUBES SHALL BE FIELD CUT TO LENGTH, PLACED IN THE HOLE AND BACKFILLED WITH ADEQUATELY COMPACTED MATERIAL EXCAVATED FROM THE HOLE.

SEE APPROVED PRODUCTS LIST FOR ACCEPTABLE E. A. T. MARKER POST.

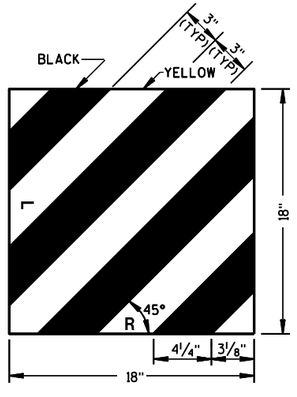
(G) ½" DIA. X 3" LAG BOLT WITH WASHER.



⑤ WOOD OFFSET BLOCK
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2

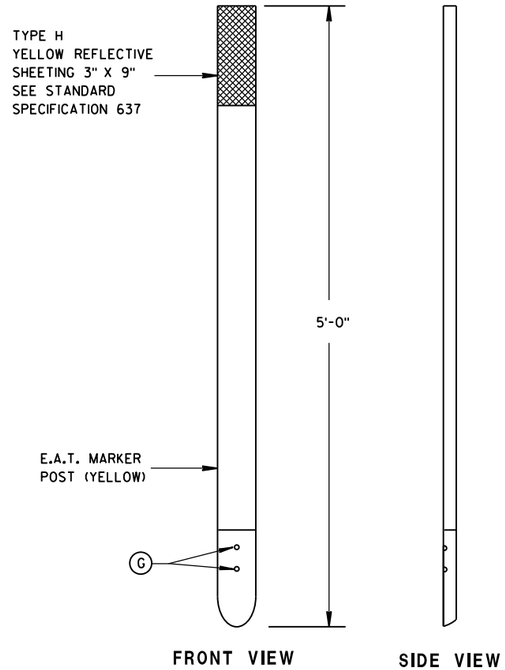


ET-2000 PLUS ONLY



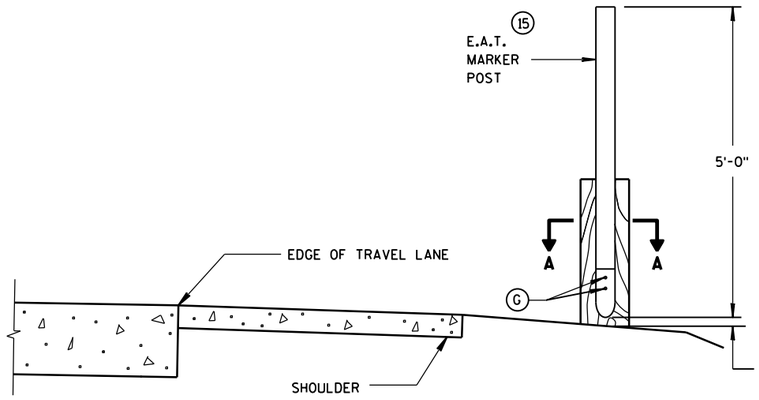
ET-2000 AND SKT-350

⑭ REFLECTIVE SHEETING DETAILS

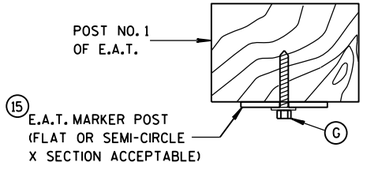


FRONT VIEW SIDE VIEW

⑮ E.A.T. MARKER POST



TYPICAL INSTALLATION OF E.A.T.
MARKER POST BACKSIDE OF POST NO. 1
(E.A.T. AND RAIL REMOVED FOR CLARITY)



SECTION A-A

STEEL PLATE BEAM GUARD
ENERGY ABSORBING TERMINAL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

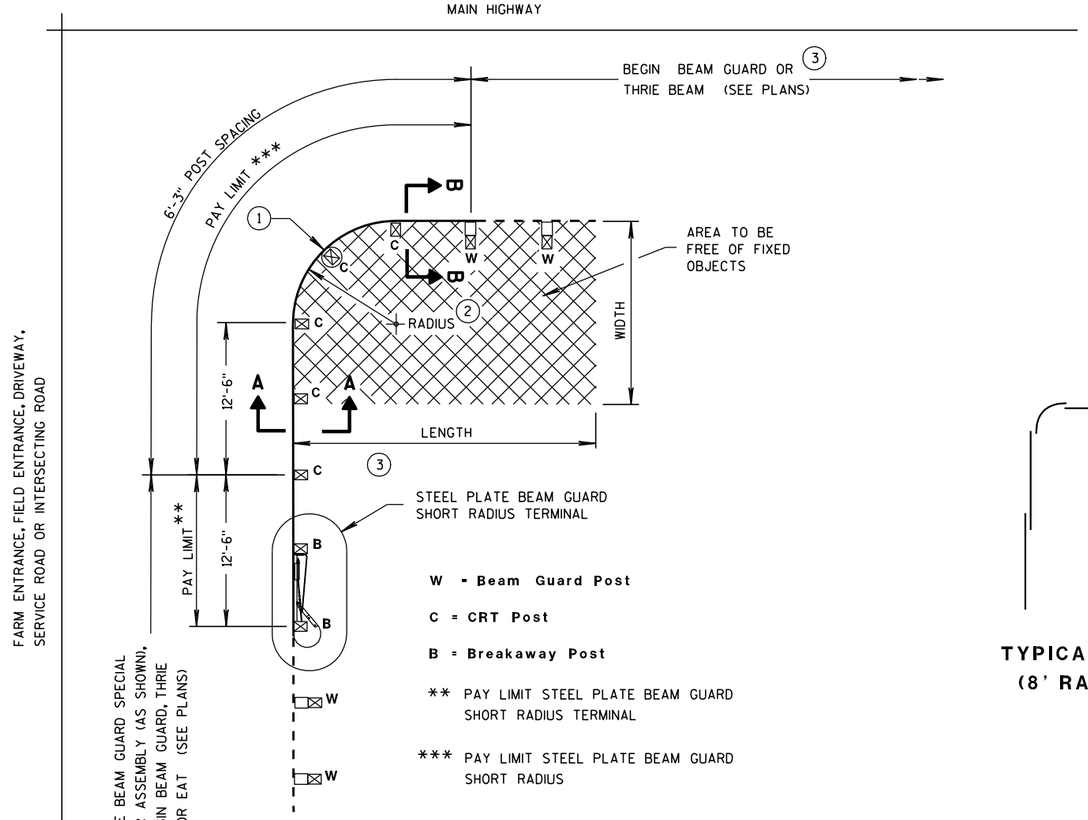
APPROVED
June 2014 — DATE
/S/ Jerry H. Zoda — ROADWAY STANDARDS ENGINEER
FHWA

6

S.D.D. 14 B 24-8c

6

S.D.D. 14 B 24-8c



W = Beam Guard Post

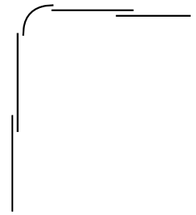
C = CRT Post

B = Breakaway Post

** PAY LIMIT STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL

*** PAY LIMIT STEEL PLATE BEAM GUARD SHORT RADIUS

TYPICAL LAYOUT
(8' RADIUS SHOWN)



TYPICAL LAP SPLICES
(8' RADIUS SHOWN)

GENERAL NOTES

ALL ANGLES, CHANNELS, AND PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A36 AND THE STRUCTURAL TUBING SHALL CONFORM TO ASTM A 500. WELDING SHALL MEET THE CURRENT REQUIREMENTS OF THE AMERICAN WELDING SOCIETY STRUCTURAL WELDING CODE ANSI/AWS D1.1. ALL STRUCTURAL STEEL SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A 123. PUNCHING, DRILLING, CUTTING, OR WELDING WILL NOT BE PERMITTED AFTER GALVANIZING. FURNISH AND INSTALL HARDWARE PER STANDARD SPECIFICATION 614.2. UNLESS NOTED OTHERWISE.

SHOP BEND CURVED RAIL SECTIONS.

SEE STANDARD DETAIL DRAWING 14 B 15 FOR OTHER DETAIL.

(1) ON THE 8 FOOT RADIUS INSTALLATION, DO NOT INSTALL BUTTON HEAD BOLT AT CENTER CRT POST.

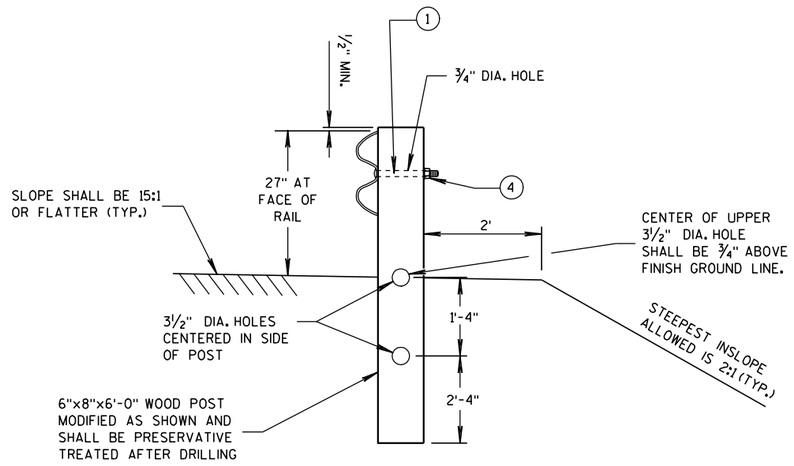
(2) RADIUS FROM 8' - 36'. SEE PLAN.

(3) HEIGHT TRANSITION MAY BE REQUIRED. SEE PLAN OR PROJECT ENGINEER.

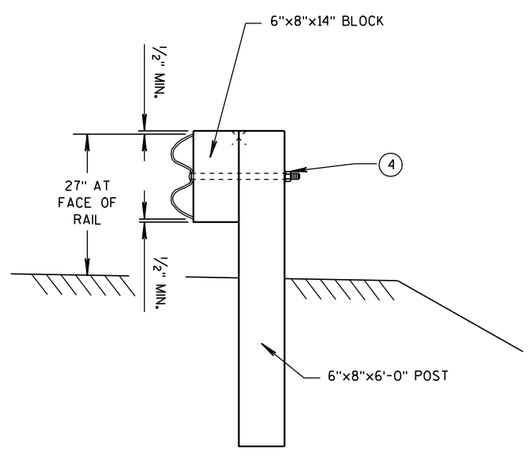
(4) 5/8" ø X 1'-6" BUTTON HEAD BOLT AND RECESS NUT WITH ROUND WASHER UNDER NUT.

RADIUS	NUMBER OF CRT POSTS	*NUMBER AND LENGTH OF CURVED RAILS	REQUIRED AREA FREE OF FIXED OBJECTS (LENGTH x WIDTH)
8'	5	1 at 12.5'	25' x 15'
16'	7	1 at 25'	30' x 15'
24'	9	1 at 25' and 1 at 12.5'	40' x 20'
32'	11	2 at 25'	50' x 20'

* THE NUMBER OF RAILS IS BASED ON A 90° INTERSECTION. SEE PLAN FOR NON 90° INSTALLATIONS.



SECTION A-A
(CRT POST)



SECTION B-B
(BEAM GUARD POST)

STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL

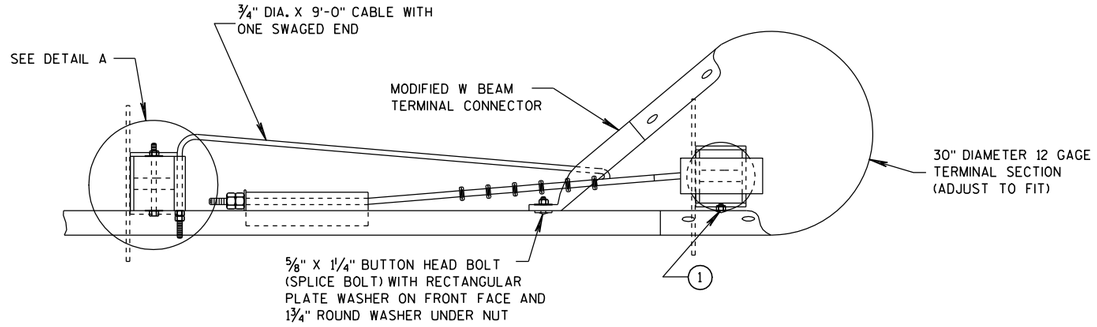
STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	

6

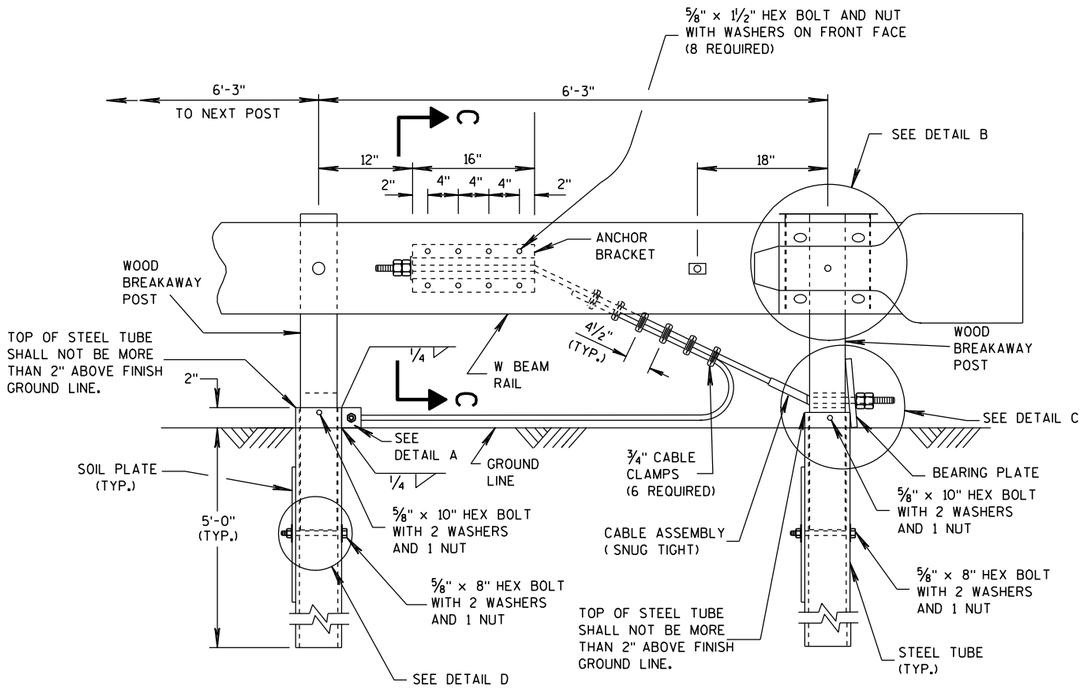
S.D.D. 14 B 27-1a

6

S.D.D. 14 B 27-1a



PLAN VIEW



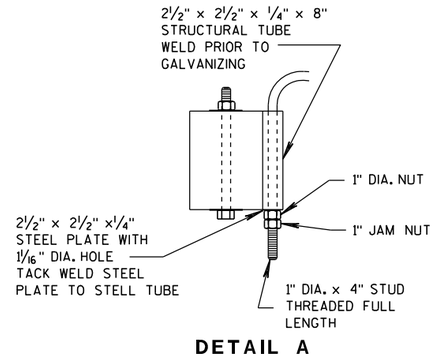
ELEVATION VIEW

STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL

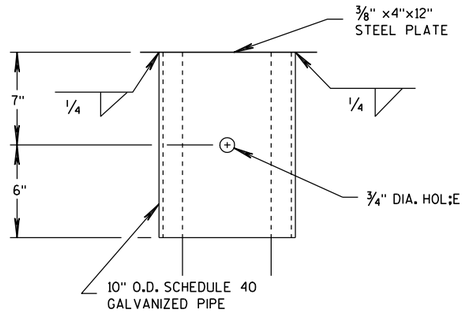
GENERAL NOTES

(1) ATTACH W BEAM RAIL TO THE STEEL PIPE WITH A ⅝" X 2" BUTTON HEAD BOLT WITH NO WASHER. CONNECTION TO THE POST IS NOT REQUIRED.

INSTALL GALVANIZED ¾" (6X19) PREFORMED WIRE OR INDEPENDENT WIRE ROPE CORE CONFORMING TO AASHTO M 30. MANUFACTURE WIRE ROPE OUT OF IMPROVED PLOW STEEL WITH A MINIMUM BREAKING STRENGTH OF 42,800 PSI.



DETAIL A



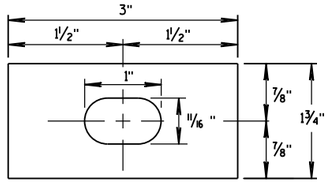
DETAIL B

(BEAM GUARD AND TERMINAL SECTION NOT SHOWN)

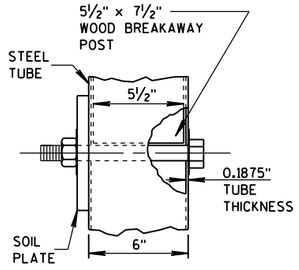
STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

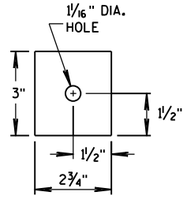
6
S.D.D. 14 B 27-1b



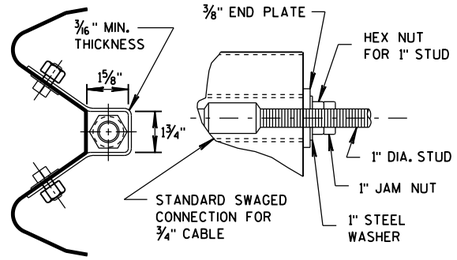
RECTANGULAR PLATE WASHER



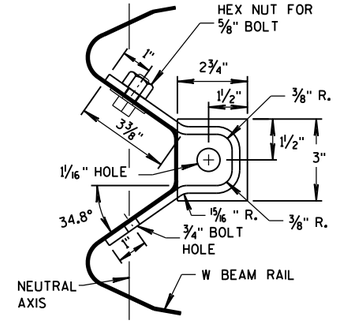
DETAIL D



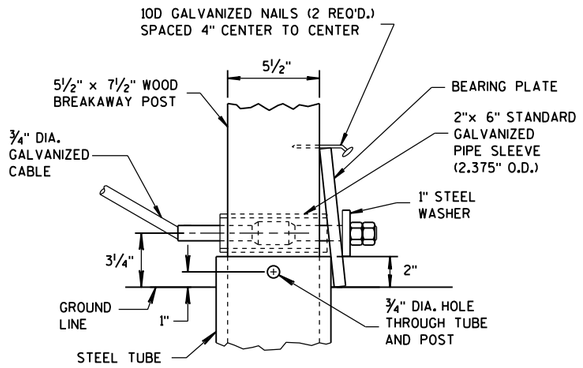
END PLATE



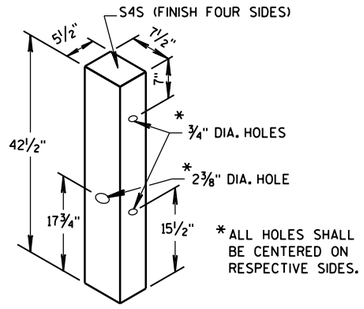
SECTION C-C
(END PLATE REMOVED)



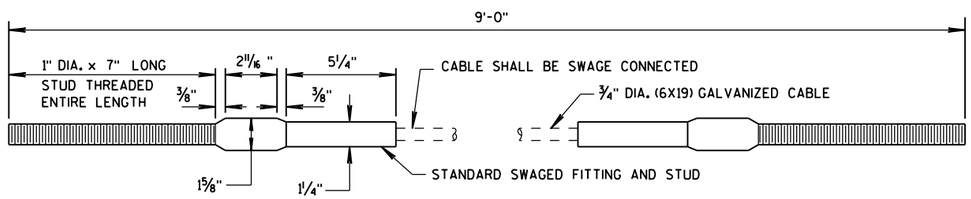
ANCHOR BRACKET



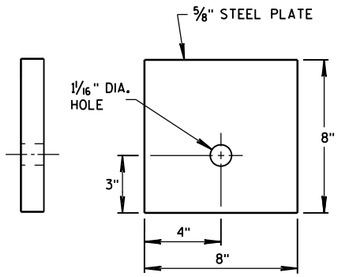
DETAIL C



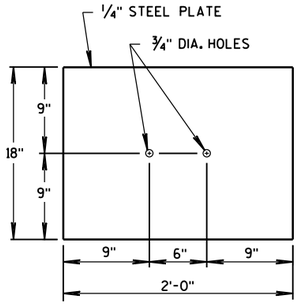
WOOD BREAKAWAY POST



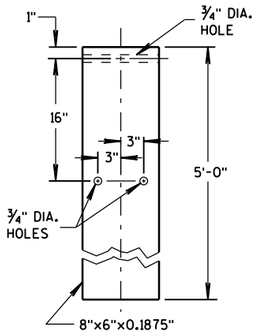
CABLE ASSEMBLY



BEARING PLATE



SOIL PLATE



STEEL TUBE

STEEL PLATE BEAM GUARD
SHORT RADIUS TERMINAL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
12/18/08 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

6

S.D.D. 14 B 27-1c

6

S.D.D. 14 B 27-1c

① WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.

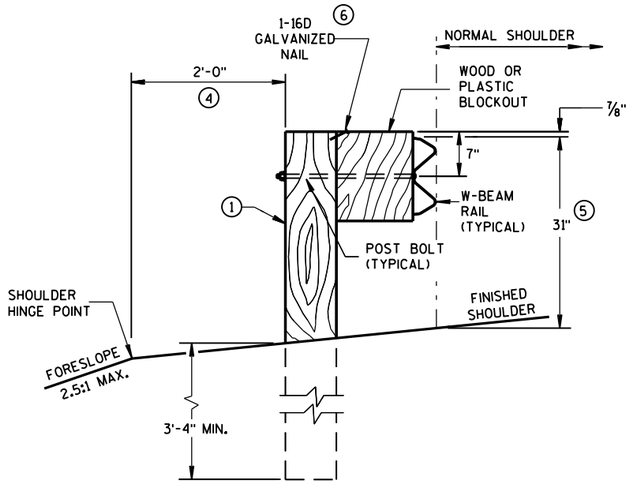
② USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.

③ IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2½ INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.

④ WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).

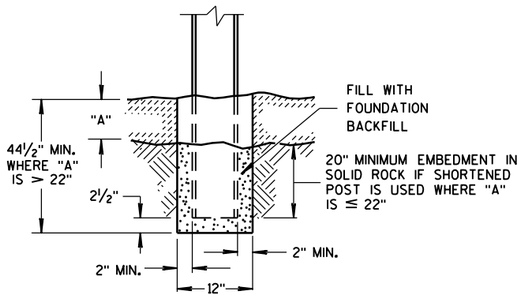
⑤ FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS ± 1". FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27¾" TO 32".

⑥ WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.



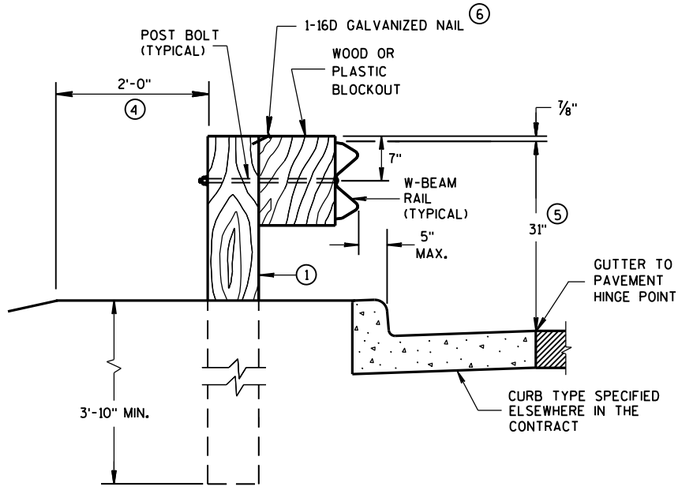
END VIEW

LOCATED ALONG A ROADWAY SHOULDER
STANDARD INSTALLATION



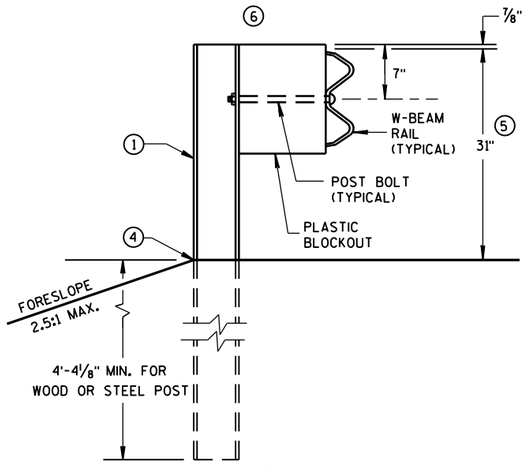
END VIEW

SETTING STEEL OR WOOD POST IN ROCK ③



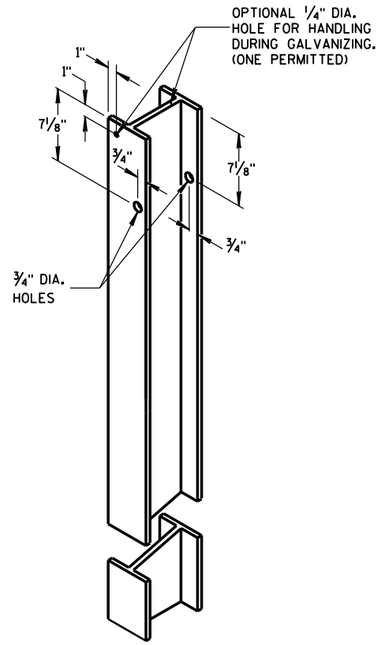
END VIEW

LOCATED ALONG A CURBED ROADWAY

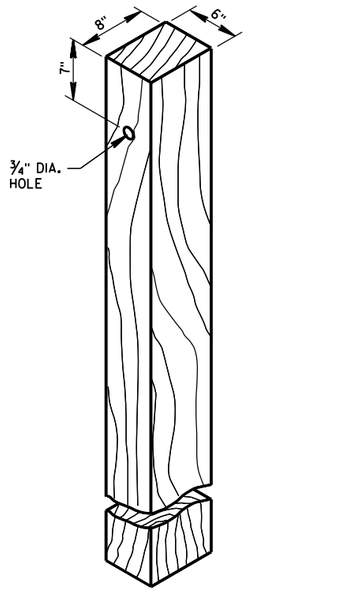


END VIEW

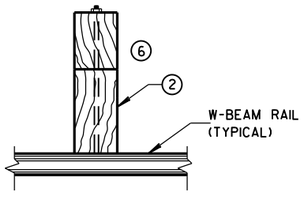
MGS LONGER POST AT HALFPOST SPACING W BEAM (K)



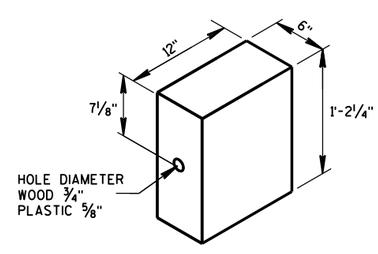
STEEL POST &
HOLE PUNCHING DETAIL
(w6X9) ①



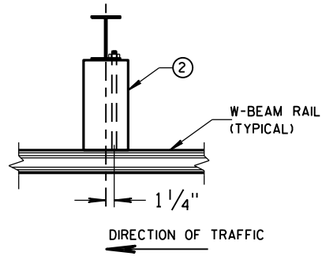
WOOD POST
(6" X 8") NOMINAL ①



PLAN VIEW
WOOD POST,
BLOCKOUT & BEAM



WOOD OR
PLASTIC BLOCKOUT ②

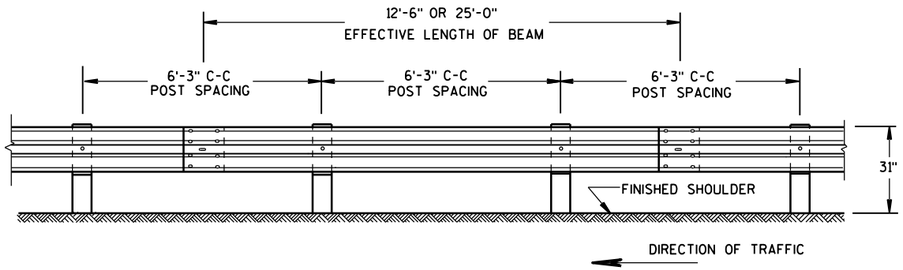


PLAN VIEW
STEEL POST,
PLASTIC BLOCKOUT & BEAM

MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL

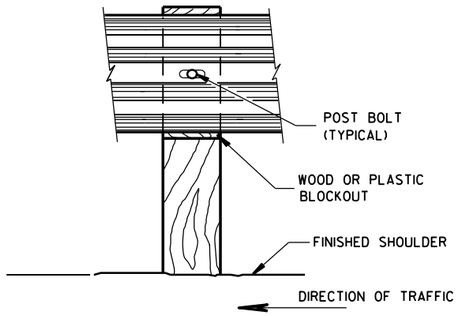
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI 77

6
S.D.D. 14 B 42-4a



FRONT VIEW

POST SPACING STANDARD INSTALLATION



FRONT VIEW AT WOOD POST

7. PROVIDE SILVER REFLECTIVE SHEETING ON ALL REFLECTORS EXCEPT THOSE LOCATED ALONG THE LEFT EDGE OF ONE-WAY ROADWAYS, WHICH SHALL BE PROVIDED WITH YELLOW REFLECTIVE SHEETING. SHEETING IS TYPE H. SEE STANDARD SPECIFICATION 637.

8. DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL. RAIL SPLICE LOCATIONS ARE THE ONLY ACCEPTABLE LOCATIONS FOR REFLECTORS.

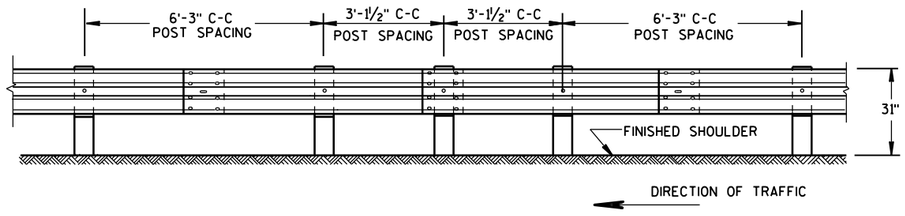
9. REVERSE EVERY OTHER REFLECTOR FOR 2-WAY VISIBILITY. THE CONTRACTOR MAY FURNISH TWO-SIDED REFLECTORS IN LIEU OF ONE-SIDED REFLECTORS.

10. PROVIDE AN ANGLE OF BEND OF 90° ± 1° FOR TWO-SIDED REFLECTORS.

11. 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.

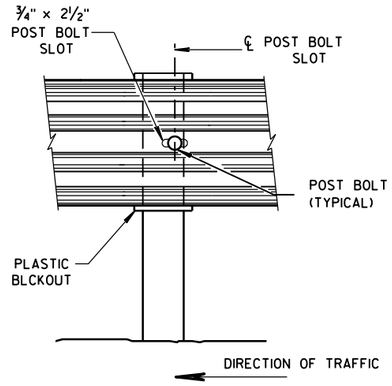
POST BOLTS ARE A 5/8" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES 5/8" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND 5/8" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.

GUARD RAIL SPLICE BOLTS ARE A 5/8" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES 5/8" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.

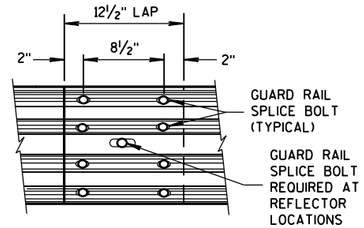


FRONT VIEW

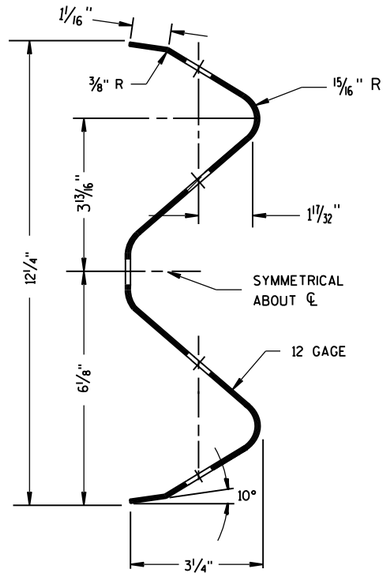
HALF POST SPACING (HS) AND HALF POST SPACING WITH LONGER POSTS (K)



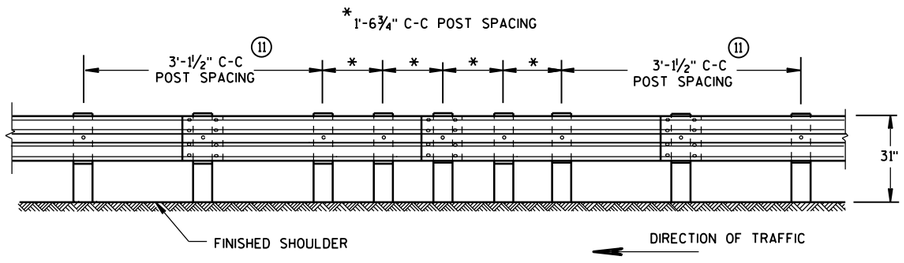
FRONT VIEW AT STEEL POST



FRONT VIEW
MID-SPAN BEAM SPLICE

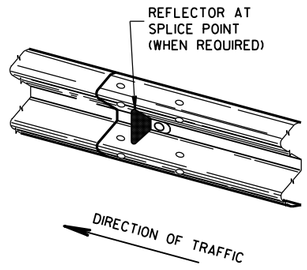


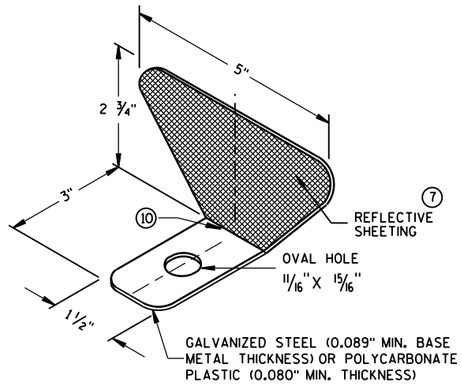
SECTION THRU W-BEAM RAIL



FRONT VIEW

QUARTER POST SPACING (QS)





ONE SIDED REFLECTOR DETAIL AND TYPICAL INSTALLATION 7

REFLECTOR SPACING 8

	BEAM GUARD LENGTH	REFLECTOR SPACING	NO. SURFACES REFLECTORIZED	MIN. NO. REFLECTORS
ONE WAY TRAFFIC	< 200'	50' C-C	1	3
	> 200'	100' C-C	1	
TWO WAY TRAFFIC	< 200'	25' C-C	1 9	6
	> 200'	50' C-C	1	
TWO WAY TRAFFIC	< 200'	50' C-C	2	3
	> 200'	100' C-C	2 10	

MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL

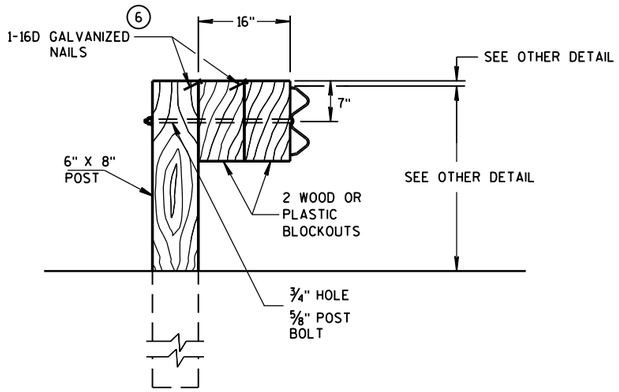
STATE OF WISCONSIN
DEPARTMENT OF TRANSPO 78

6

S.D.D. 14 B 42-4b

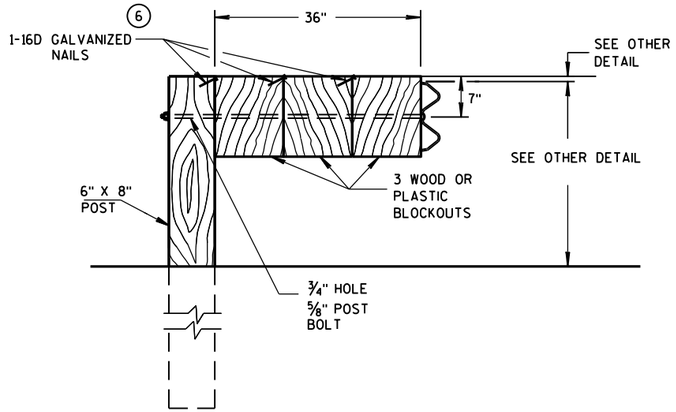
6

S.D.D. 14 B 42-4b



DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

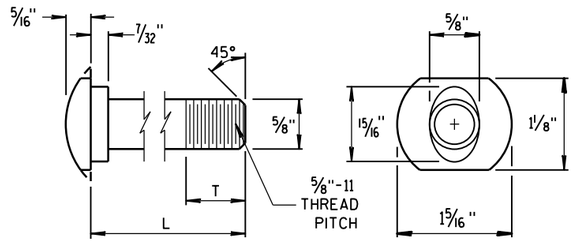


DETAIL FOR 36" BLOCKOUT DEPTH

NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.

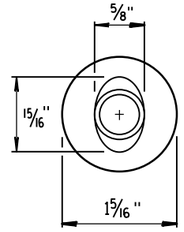
DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

NOTE: 1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 1/16".
2. IF THE BOLT EXTENDS MORE THAN 1/4" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

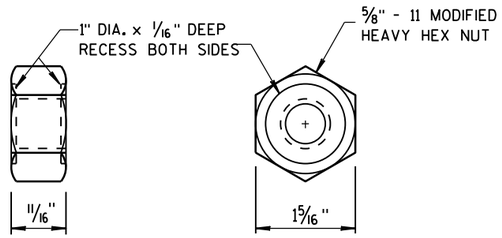


POST BOLT TABLE

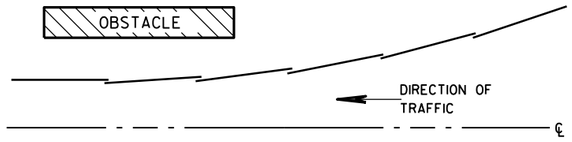
L	T (MIN.)
1 1/4"	1 1/8"
2"	1 3/4"
10"	4"
14"	4 1/16"
18"	4"
21"	4 1/16"
25"	4"



ALTERNATE BOLT HEAD

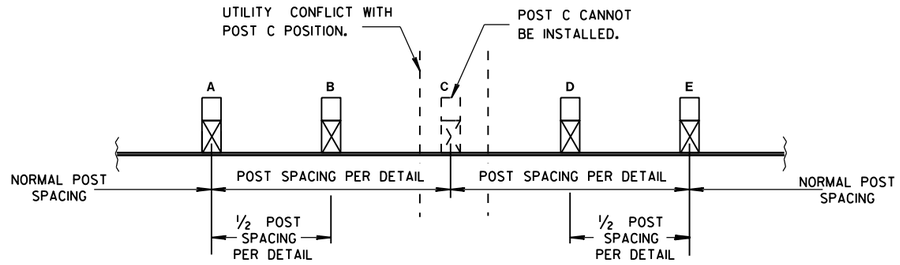


POST BOLT, SPLICE BOLT AND RECESS NUT

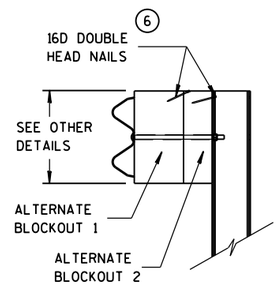


PLAN VIEW

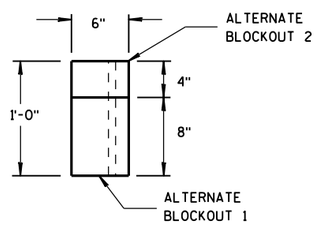
BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION



SIDE VIEW



TOP VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

6
S.D.D. 14 B 42-4c

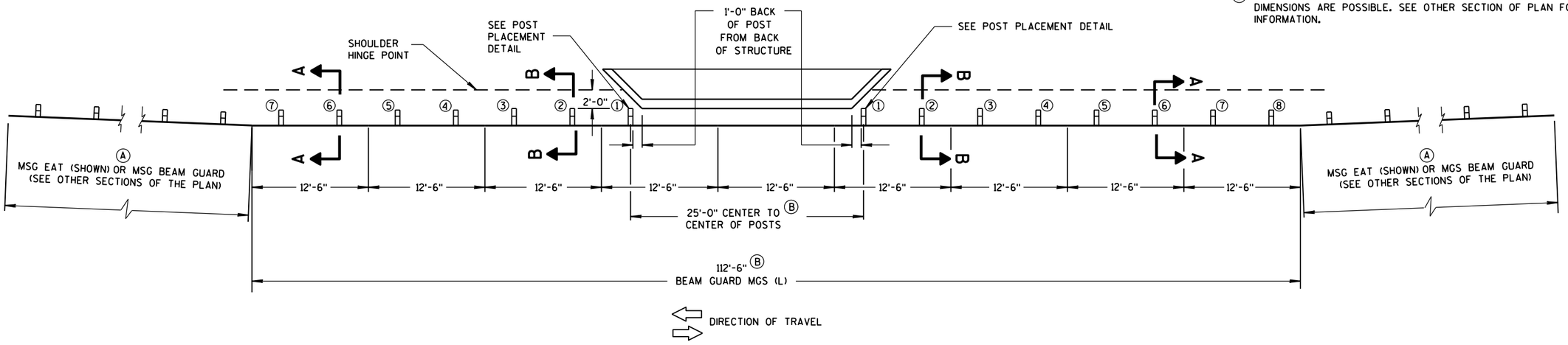
GENERAL NOTES

POSTS 1 THROUGH 3 ARE CRT POSTS.
ALL OTHER POSTS SHALL BE WOOD OR STEEL.

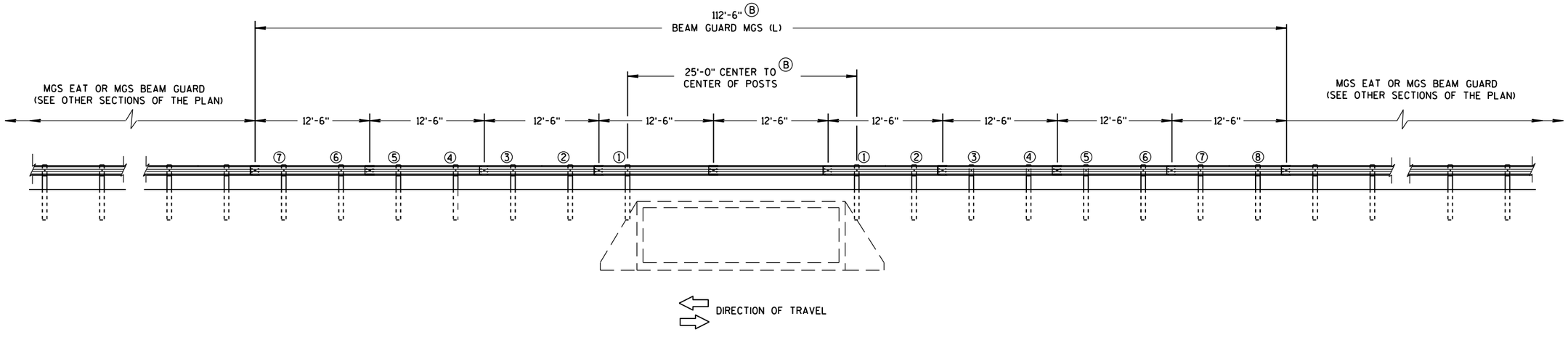
SEE SDD 14 B 42 FOR MORE DETAILS.

(A) FLARE FOR MGS EAT SHOWN. IF INSTALLING MGS NO FLARE NEEDED.

(B) VALUES SHOWN ON DRAWING REPRESENT THE MAXIMUM LENGTH. SHORTER DIMENSIONS ARE POSSIBLE. SEE OTHER SECTION OF PLAN FOR MORE INFORMATION.



PLAN VIEW



ELEVATION VIEW

MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L) TWO-WAY TRAFFIC

MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L)
STATE OF WISCONSIN DEPARTMENT OF TRANSPO

6

S.D.D. 14 B 43-3a

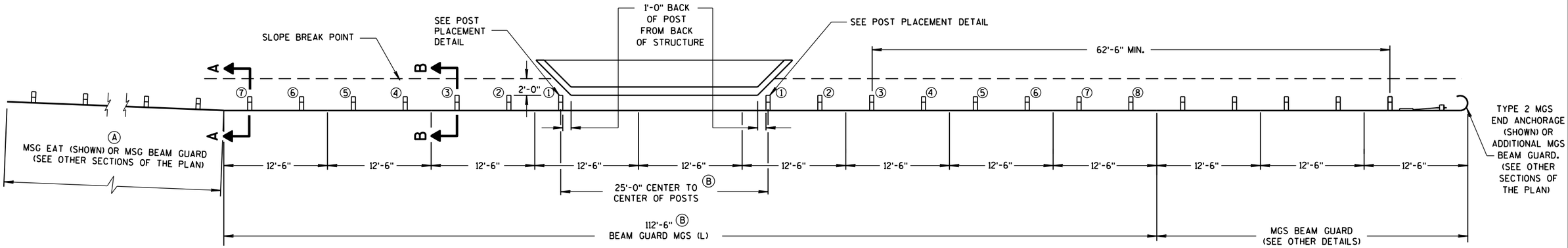
GENERAL NOTES

POSTS 1 THROUGH 3 ARE CRT POSTS.
ALL OTHER POSTS SHALL BE WOOD OR STEEL.

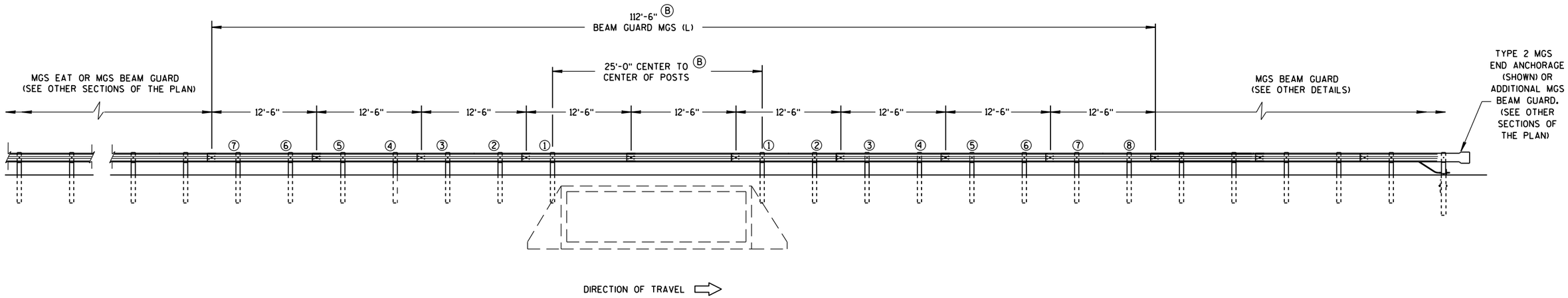
SEE SDD 14 B 42 FOR MORE DETAILS.

(A) FLARE FOR MGS EAT SHOWN. IF INSTALLING MGS NO FLARE NEEDED.

(B) VALUES SHOWN ON DRAWING REPRESENT THE MAXIMUM LENGTH. SHORTER DIMENSIONS ARE POSSIBLE. SEE OTHER SECTION OF PLAN FOR MORE INFORMATION.



PLAN VIEW



ELEVATION VIEW

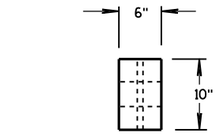
MIDWEST GUARDRAIL SYSTEM LONG SPAN MGS (L) ONE-WAY TRAFFIC

MIDWEST GUARDRAIL SYSTEM
LONG SPAN MGS (L)

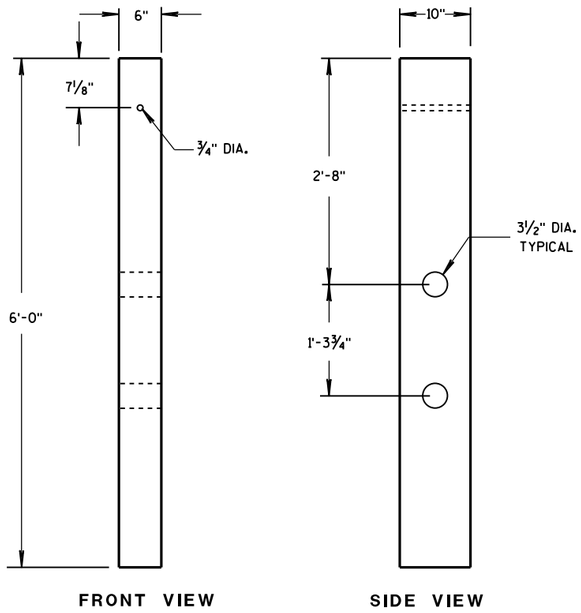
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

6

S.D.D. 14 B 43-3b

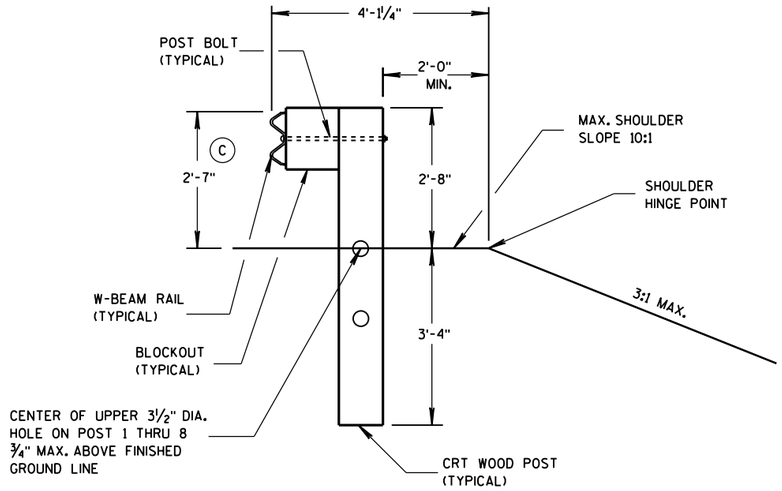


PLAN VIEW



FRONT VIEW SIDE VIEW

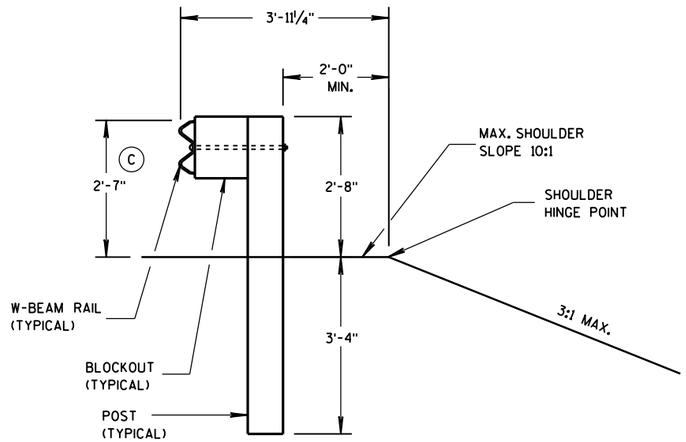
CRT WOOD POST



SECTION B-B

POSTS NO. 1-3

SEE OTHER DETAILS



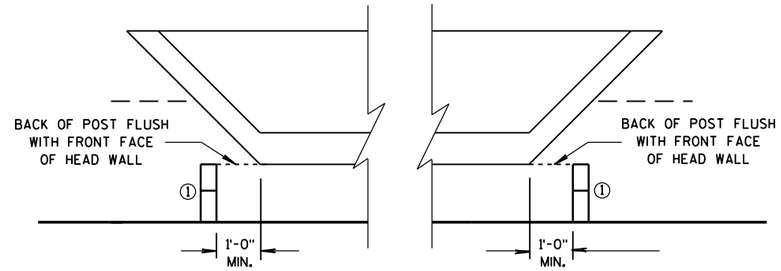
SECTION A-A

POSTS NO. 4-8

SEE OTHER DETAILS

GENERAL NOTES

(C) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".



POST PLACEMENT DETAIL

MIDWEST GUARDRAIL SYSTEM
LONG SPAN MGS (L)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5/10/2013
DATE

/S/ Jerry H. Zogg
ROADWAY STANDARDS ENGINEER

FHWA

6

S.D.D. 14 B 43-3c

GENERAL NOTES

(A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE (HPL), AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.

(B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED.

(C) DIFFERENT MANUFACTURES REQUIRE DIFFERENT PERFORATED W-BEAM RAIL END PANELS. SEE MANUFACTURES INFORMATION.

(D) THE TOP OF THE STEEL TUBE ON POST 1 AND POST 2 SHALL NOT BE MORE THAN 3" ABOVE THE FINISH GROUND ELEVATION.

(F) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF-TAPPING SCREWS. ONE SCREW PER CORNER.

(G) ½" DIAMETER X 3" LONG LAG BOLT AND WASHER.

(H) HARDWARE VARIES BETWEEN DIFFERENT MANUFACTURES. SEE MANUFACTURE'S DRAWING FOR INFORMATION.

(I) DIMENSIONS MAY VARY. SEE MANUFACTURE'S INFORMATION.

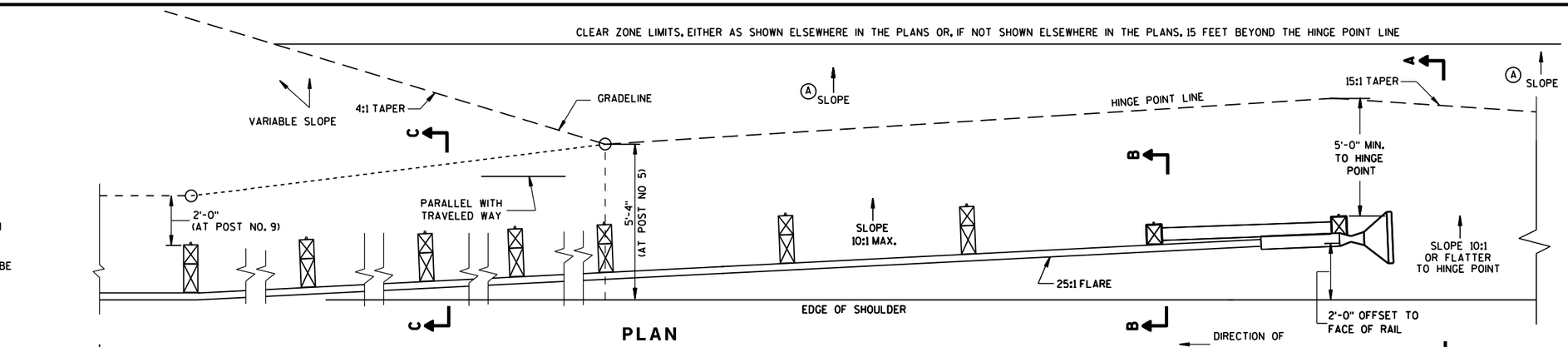
SEE SDD 14B42 FOR MORE INFORMATION.

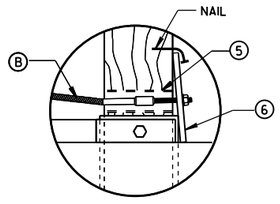
* DO NOT ATTACH BLOCKOUTS TO POSTS 1 AND 2.

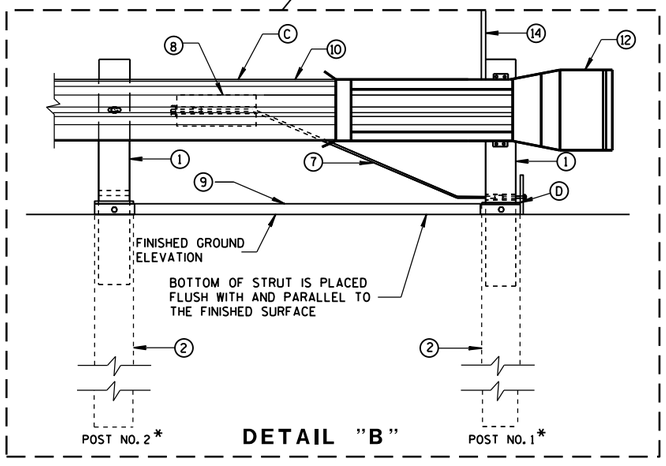
DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

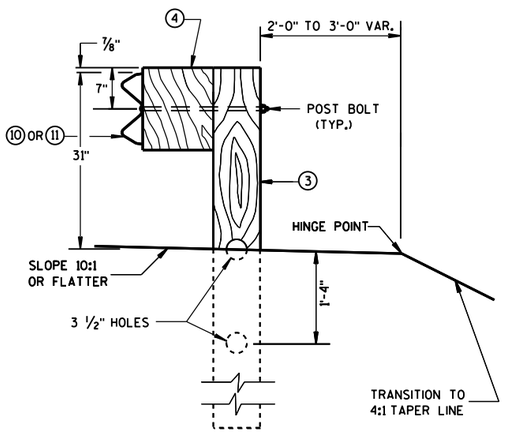
W-BEAM RAIL SPLICES ARE LOCATED AT POST NUMBER 3, AND BETWEEN POST 5 AND 6, BETWEEN POSTS 7 AND 8, AND MIDDLE OF THE SPAN AFTER POST 9.

THE CENTER OF THE UPPER 3½" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE.

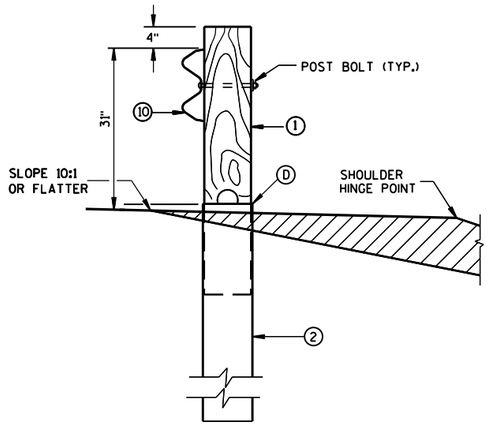








SECTION C-C
TYPICAL AT POST NOS. 3-9



SECTION B-B
TYPICAL AT POST NO. 2*

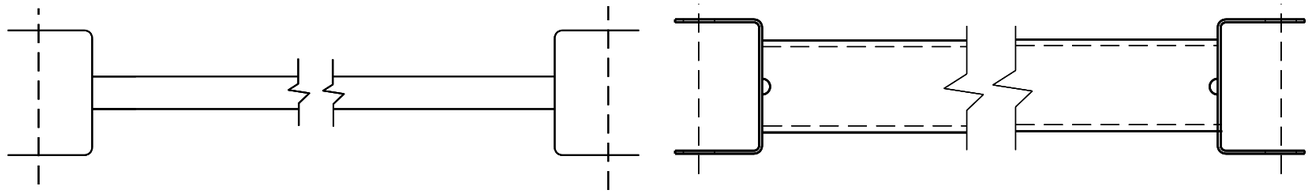
SECTION A-A
TYPICAL AT POST NO. 1*

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

S.D.D. 14 B 44-2a

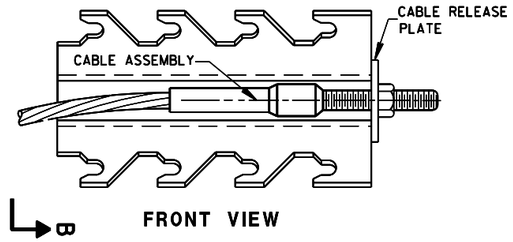
S.D.D. 14 B 44-2a



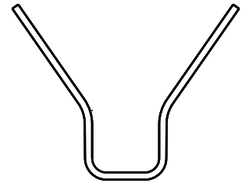
GENERIC GROUND STRUT (9) (H)

BILL OF MATERIALS

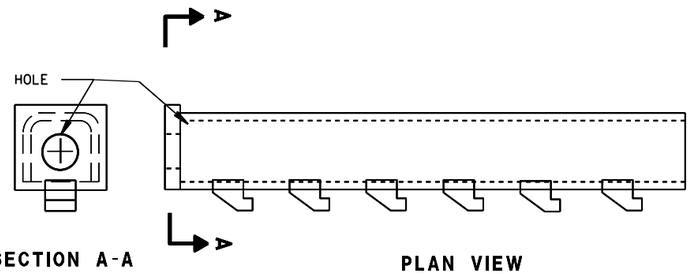
PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.
(1)	WOOD BREAKAWAY POST
(2)	6" X 8" X 0.188", 6'-0" LONG FOUNDATION TUBE AT POSTS 1 AND 2
(3)	WOOD CRT
(4)	WOOD BLOCKOUT
(5)	PIPE SLEEVE
(6)	BEARING PLATE
(7)	BCT CABLE ASSEMBLY
(8)	ANCHOR CABLE BOX
(9)	GROUND STRUT
(10)	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
(11)	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
(12)	END SECTION EAT
(13)	0.040" ALUMINUM SHEET WITH REFLECTIVE SHEETING TYPE F PER SECTION 637 OF THE STANDARD SPECIFICATIONS
(14)	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)



FRONT VIEW



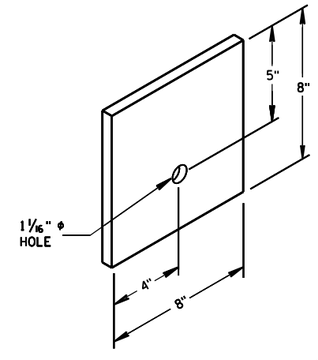
SECTION B-B



SECTION A-A

PLAN VIEW

GENERIC ANCHOR CABLE BOX (8) (H)



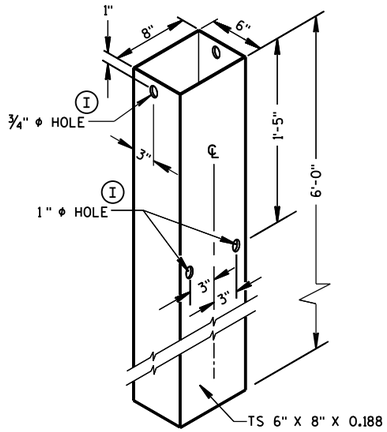
BEARING PLATE (6)

MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)

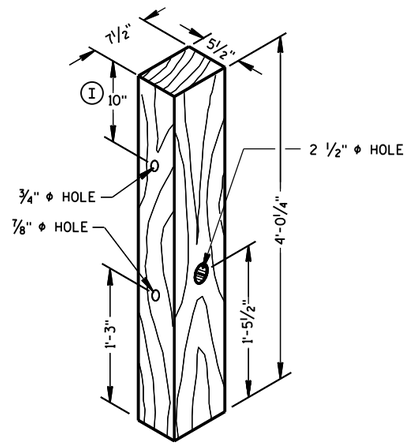
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 14 B 44-2b

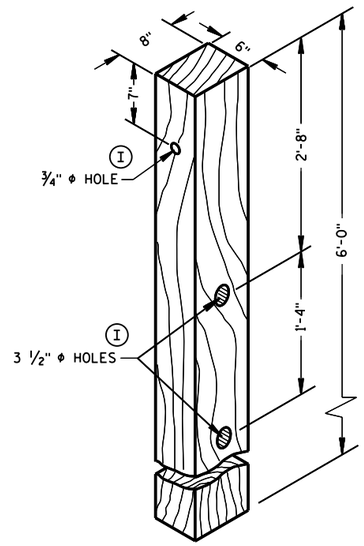


FOUNDATION TUBE (2)



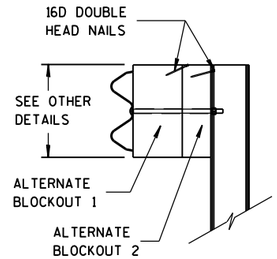
POSTS NUMBER 1 AND 2

WOOD BREAKAWAY POST (1)

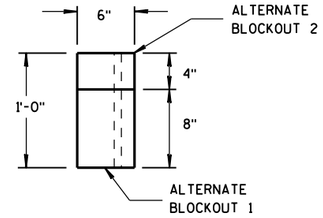


POSTS NUMBER 3-9

WOOD CRT POST (3)

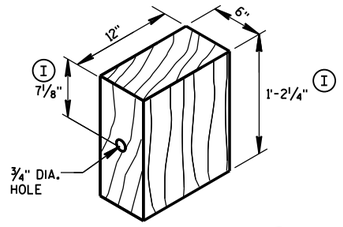


SIDE VIEW



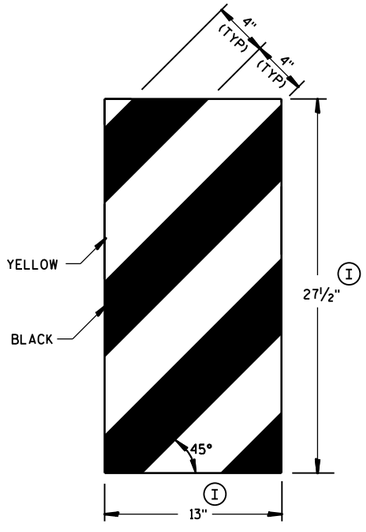
TOP VIEW

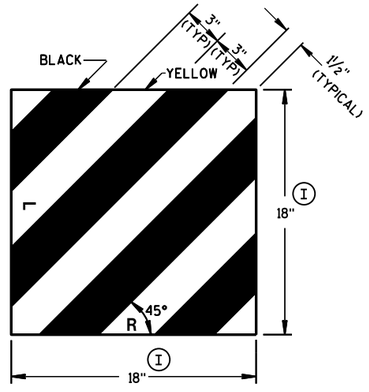
ALTERNATE WOOD BLOCKOUT DETAIL



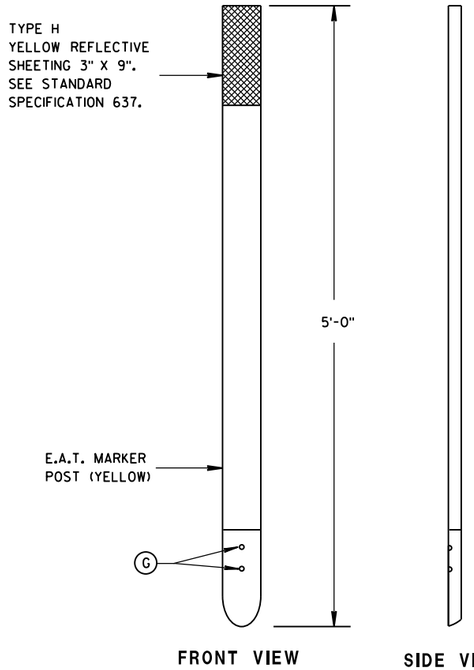
WOOD BLOCKOUT (4)

REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2





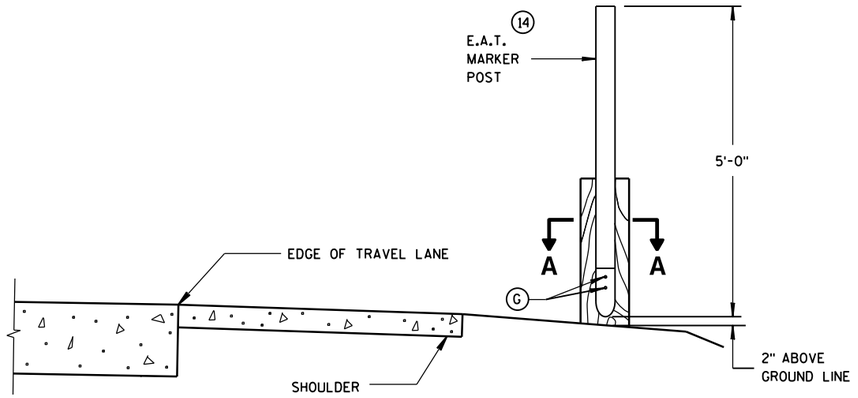
GENERIC REFLECTIVE SHEETING (13) (H)



FRONT VIEW

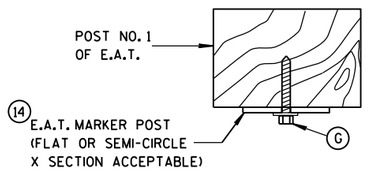
SIDE VIEW

E.A.T. MARKER POST (14)



TYPICAL INSTALLATION OF E.A.T. MARKER POST BACKSIDE OF POST NO. 1

(E.A.T. AND RAIL REMOVED FOR CLARITY)



SECTION A-A

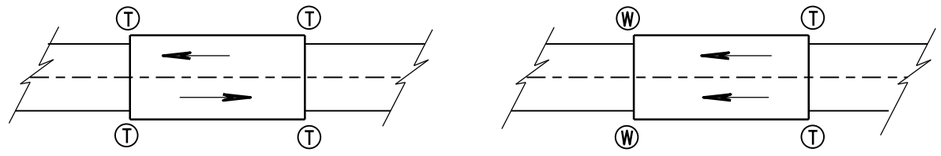
MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2014 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

6

S.D.D. 14 B 44-2c



TWO WAY TRAFFIC

ONE WAY TRAFFIC

Ⓣ THRIE BEAM CONNECTION

Ⓦ W-BEAM CONNECTION WHEN REQUIRED

TYPICAL LOCATIONS OF THRIE BEAM AND W-BEAM CONNECTIONS TO BRIDGE

GENERAL NOTES

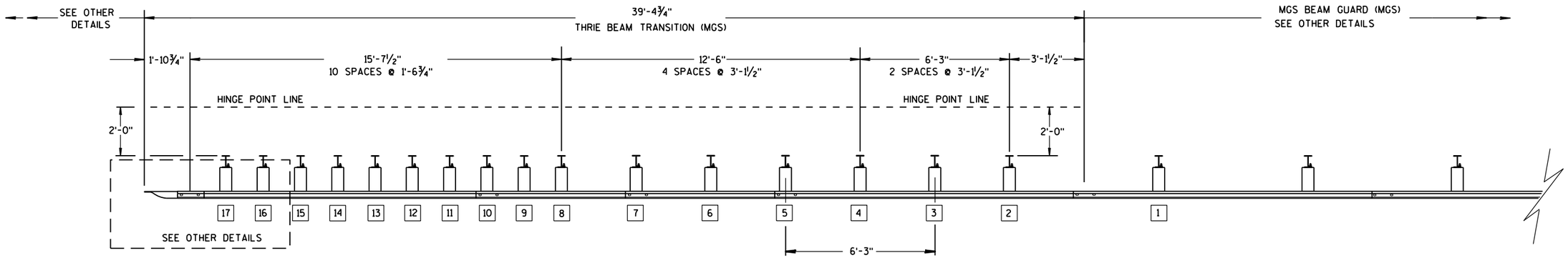
IF ROCK IS ENCOUNTERED, REMOVE ROCK TO FULL DEPTH OF POST PLUS 2½", AND 12" DIAMETER AROUND POST. SEE 14B42 FOR MORE DETAILS.

TRANSITION USES STEEL POSTS ONLY.

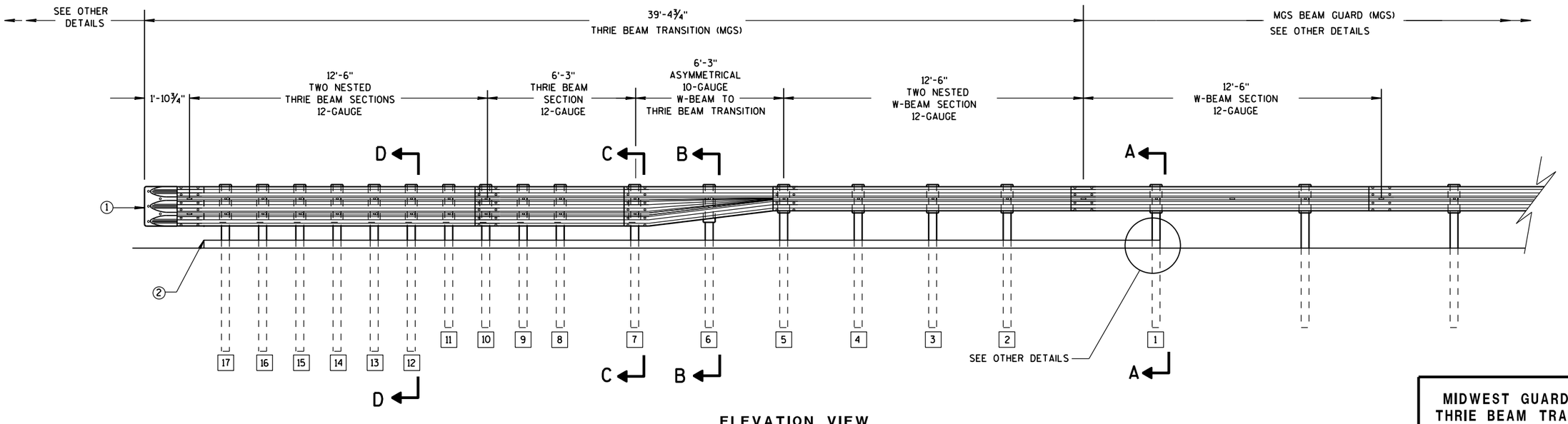
SEE STANDARD DETAIL DRAWING 14 B 42 FOR MORE INFORMATION.

① BRIDGE RAILING TYPE "W" DOES NOT REQUIRE A TERMINAL CONNECTOR.

② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.



PLAN VIEW



ELEVATION VIEW

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

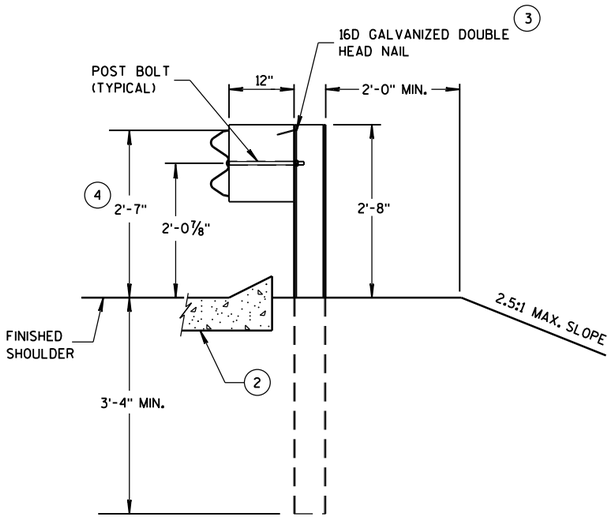
S.D.D. 14 B 45-4a

GENERAL NOTES

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE_FEATURE SEE PLAN FOR INFORMATION.

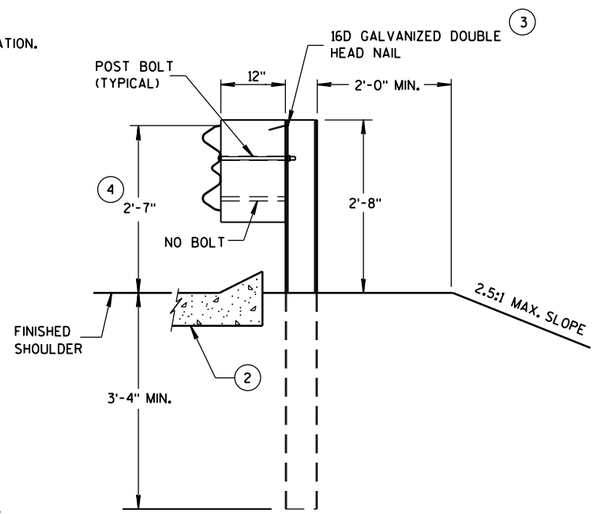
(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 10D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".



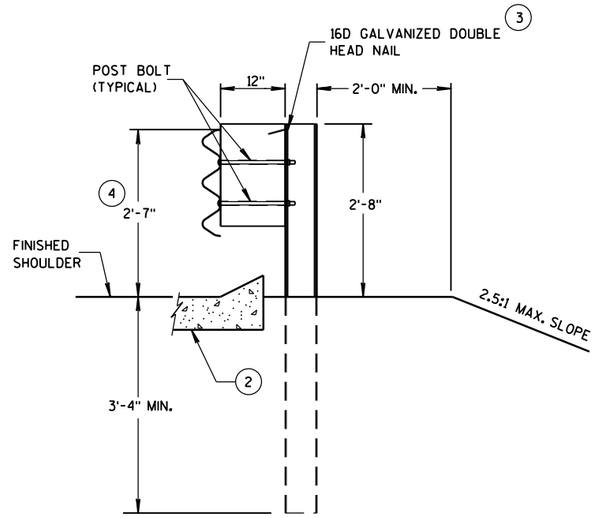
SECTION A-A

POSTS 1-5



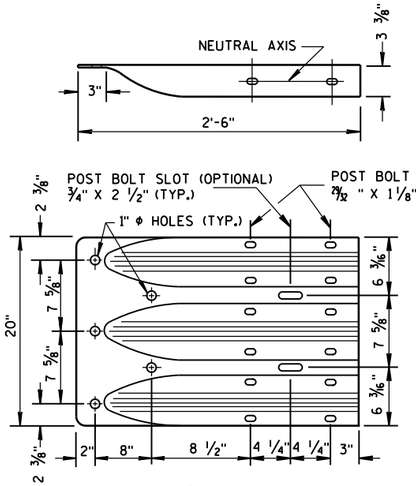
SECTION B-B

POST 6



SECTION C-C

POSTS 7-11



THRIE BEAM TERMINAL CONNECTOR

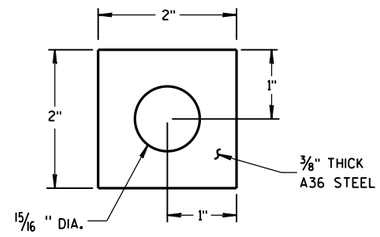
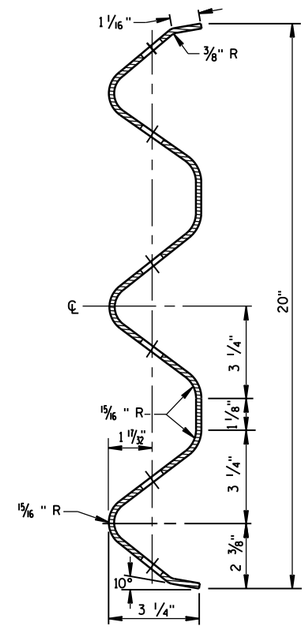
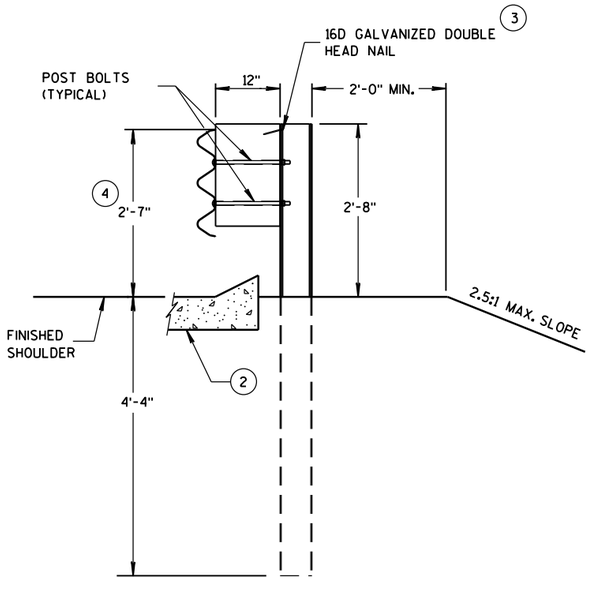


PLATE WASHER DETAIL

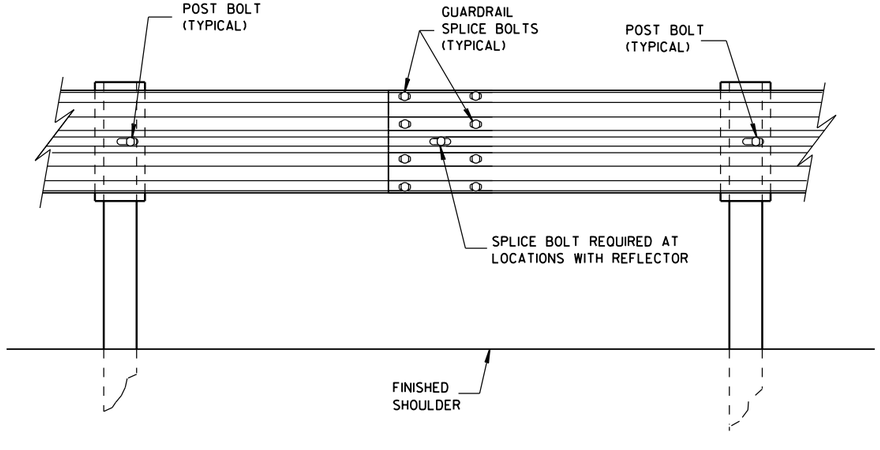


SECTION THRU THRIE BEAM RAIL ELEMENT



SECTION D-D

POSTS 12-17



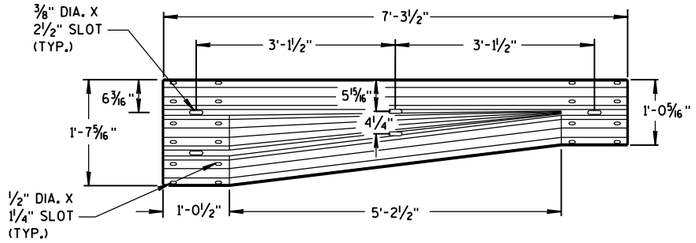
SPLICE DETAIL

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

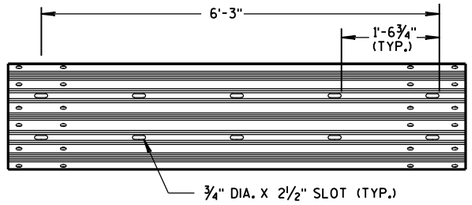
STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

6

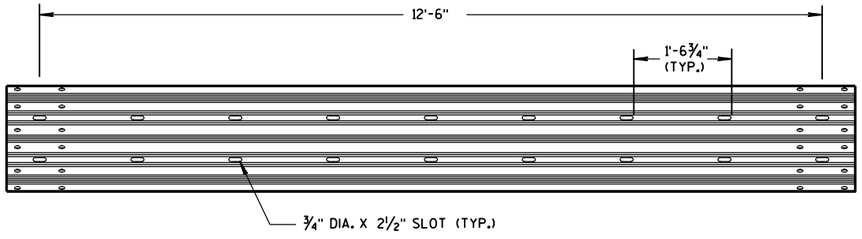
S.D.D. 14 B 45-4b



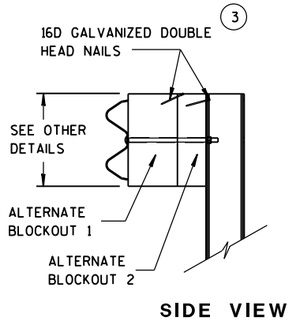
W-BEAM TO THRIE BEAM TRANSITION SECTION



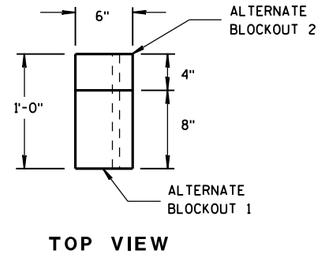
6'-3" THRIE BEAM SECTION



12'-6" THRIE BEAM SECTION

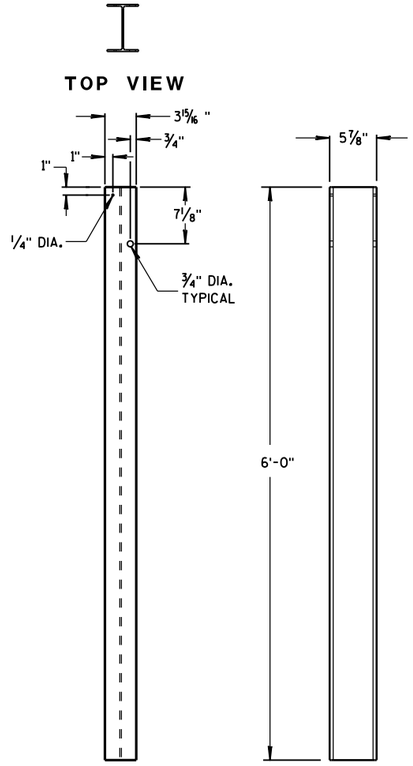


SIDE VIEW

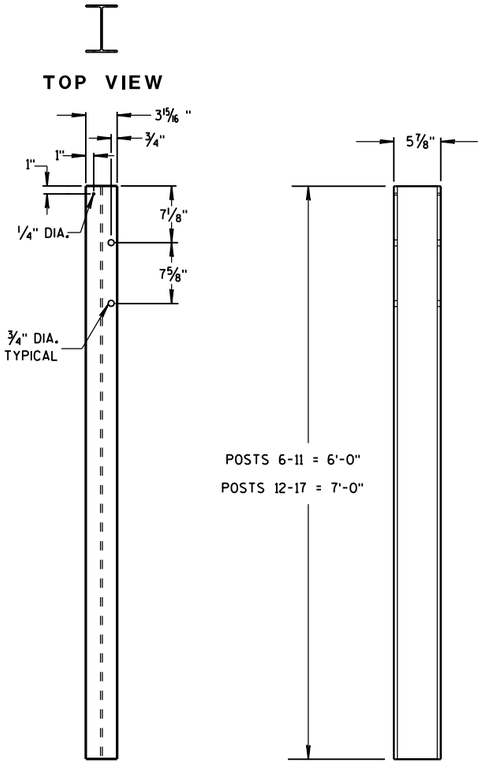


TOP VIEW

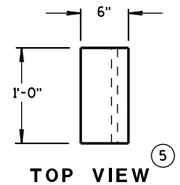
ALTERNATE WOOD BLOCKOUT DETAIL



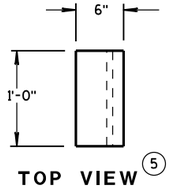
STEEL POSTS 1-5



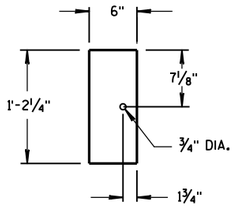
STEEL POSTS 6-17



TOP VIEW

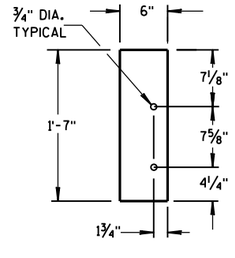


TOP VIEW



FRONT VIEW

BLOCKOUT POSTS 1-5



FRONT VIEW

BLOCKOUT POSTS 6-17

GENERAL NOTES

STEEL POSTS ARE W6X9 OR W6X8.5.

BOLT HOLES FOR POST ARE ON FRONT AND OF SIDE OF POST.

(3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

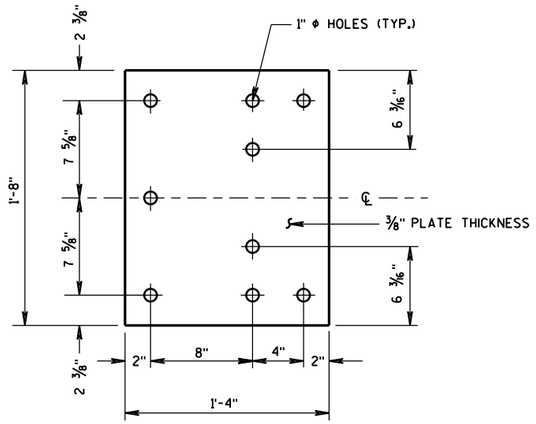
(5) WOOD BLOCKS MAY BE CONSTRUCTED OUT OF 2 WOOD BLOCKS. SEE ALTERNATE WOOD BLOCK DETAIL.

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

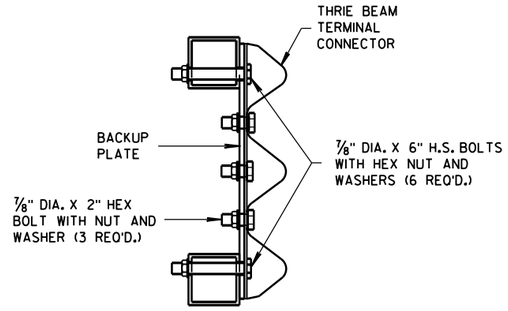
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

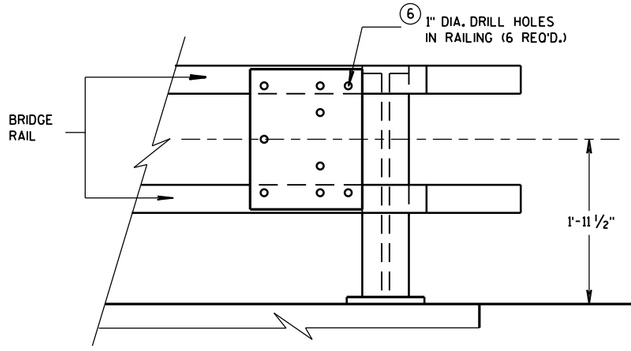
S.D.D. 14 B 45-4c
S.D.D. 14 B 45-4c



BACK-UP PLATE DETAIL



SECTION J-J

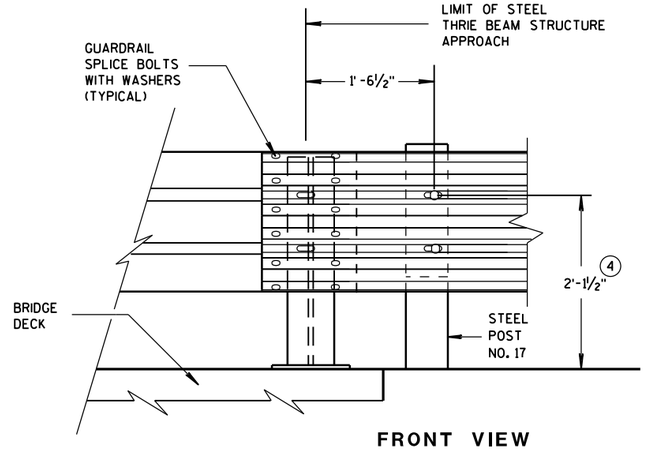


BACK-UP PLATE MOUNTING ONTO BRIDGE RAILING

GENERAL NOTES

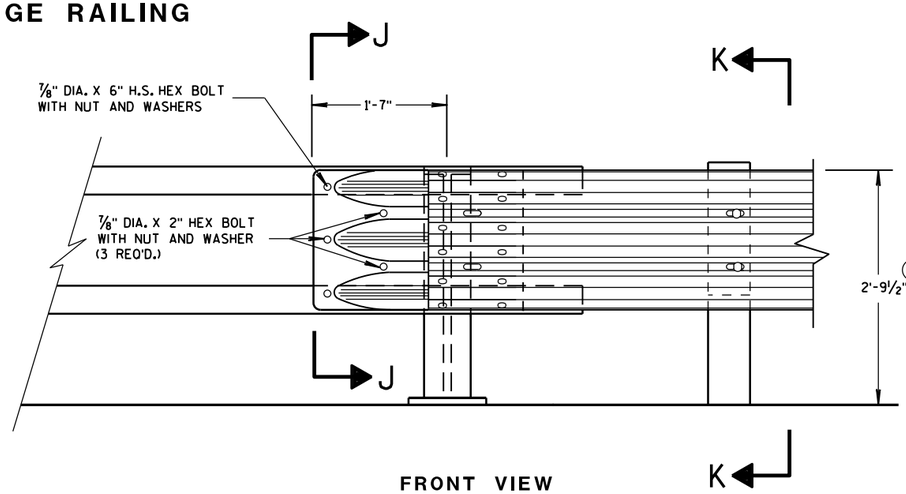
④ TOLERANCE FOR TOP OF BEAM IS ± 1".

⑥ DRILLING HOLES THROUGH THE PAPRPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

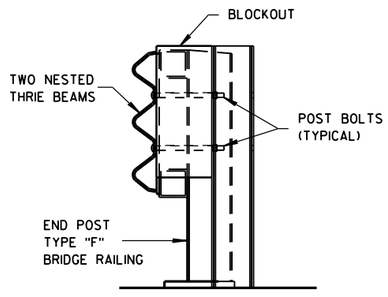


FRONT VIEW

THRIE BEAM CONNECTION TO STEEL RAILING TYPE "W"



FRONT VIEW



SECTION K-K

THRIE BEAM CONNECTION TO TUBULAR RAILING TYPE "F"

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

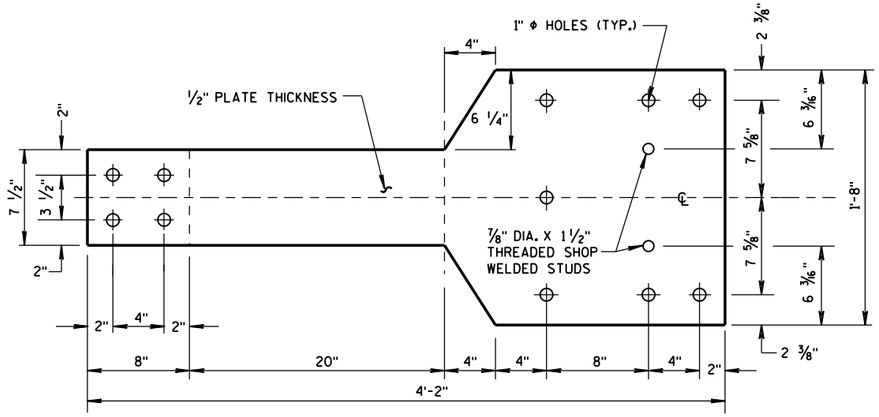
APPROVED
June, 2015 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA — NT

S.D.D. 14 B 45-4g

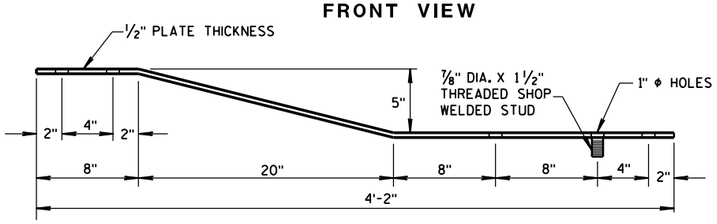
S.D.D. 14 B 45-4g

GENERAL NOTES

(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".

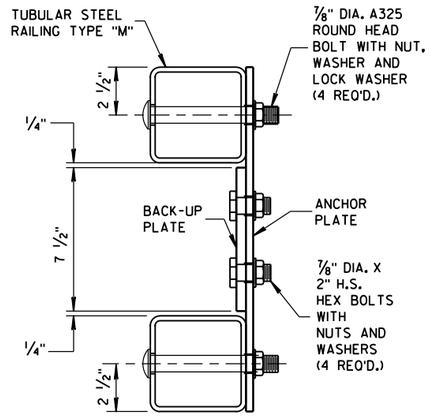


FRONT VIEW

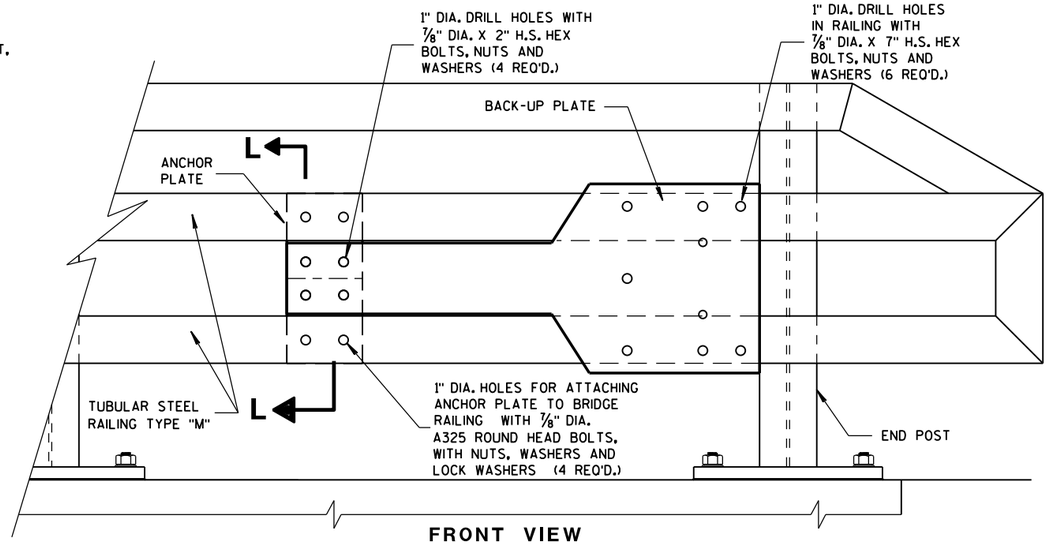


PLAN VIEW

BACK-UP PLATE DETAIL, TYPE "M"

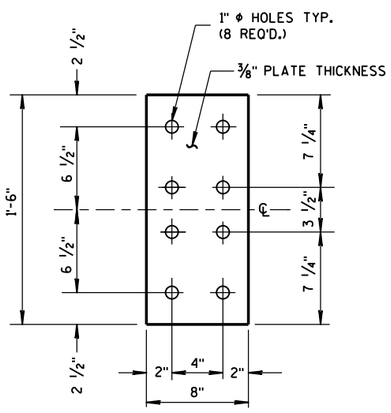


SECTION L-L



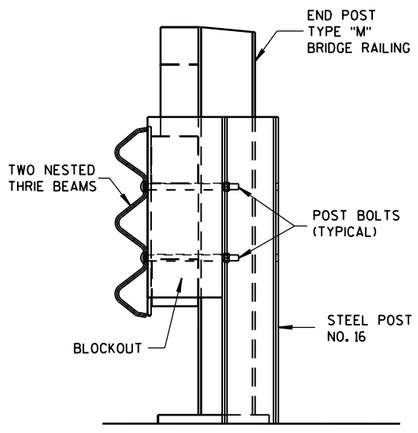
FRONT VIEW

ANCHOR AND BACK-UP PLATE MOUNTING TO BRIDGE RAILING, TYPE "M"

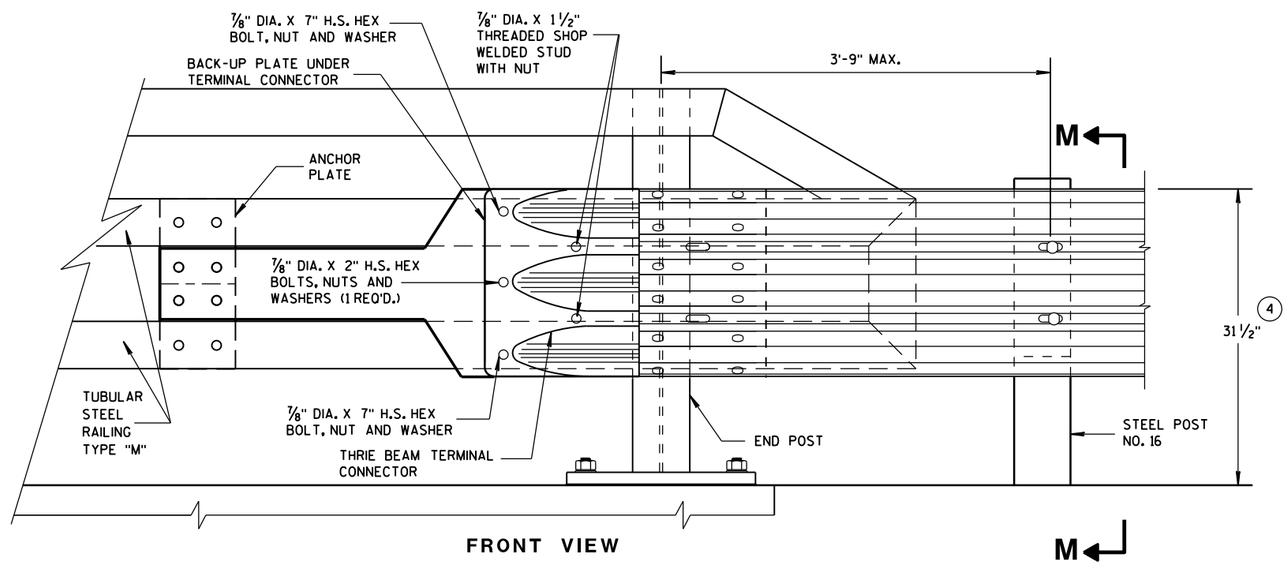


FRONT VIEW

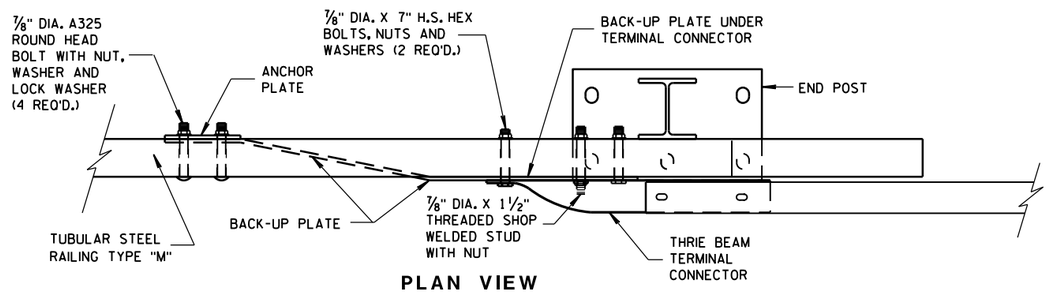
ANCHOR PLATE DETAIL, TYPE "M"



SECTION M-M



FRONT VIEW



PLAN VIEW

THRIE BEAM CONNECTION TO TUBULAR RAILING, TYPE "M"

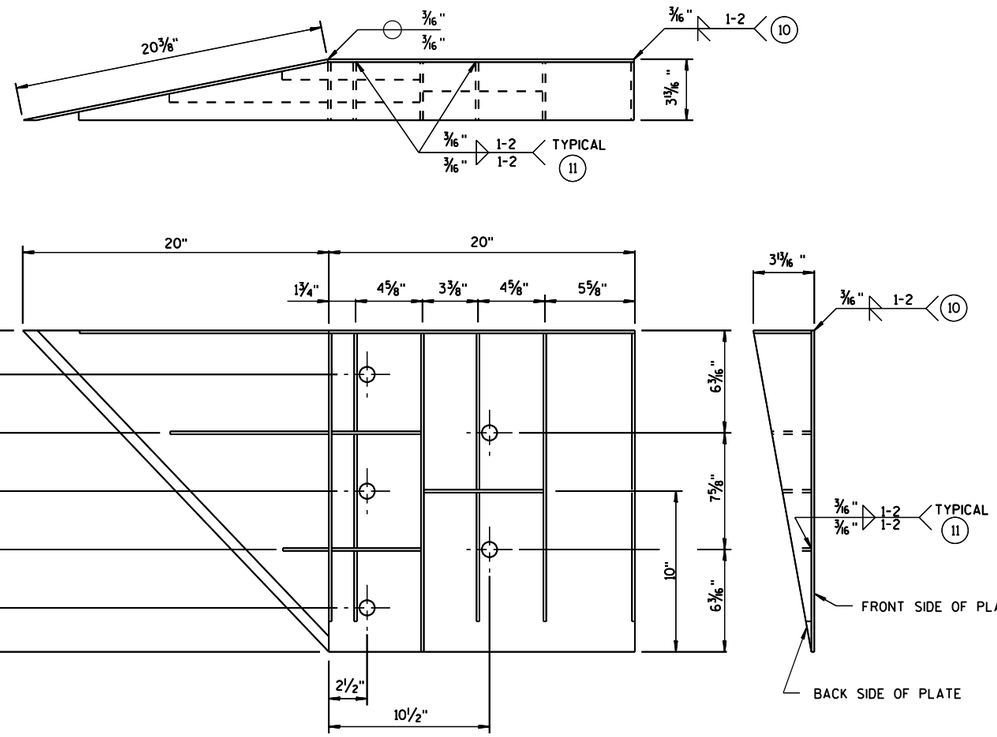
MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2015 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

6

S.D.D. 14 B 45-4h



WELDING INSTRUCTION

(VIEWED FROM BACK SIDE OF PLATE)

GENERAL NOTES

COVER PLATE PANELS ARE 3/16" THICK.

ALL STIFFENERS ARE 1/4" THICK.

CONNECTOR PLATE SHALL BE FABRICATED FROM ASTM GRADE A36 STEEL AND GALVANIZED.

FOR GALVANIZED REQUIREMENTS, SEE SECTION 614 OF THE STANDARD SPECIFICATIONS.

ALL HOLE DIAMETERS SHALL BE 1".

FOR OPPOSITE SIDE INSTALLATION MIRROR DRAWINGS.

(10) STIFFENERS LOCATED AT THE OUTSIDE EDGES OF THE COVER PLATES SHALL BE WELDED AS FOLLOWS:
SINGLE BEVEL GROOVE WELD ON EXTERNAL SIDES AND 3/16" FILLET WELD BY 1" LONG SPACED AT 2" ON INTERNAL SIDES.

(11) STIFFENERS LOCATED ON THE INSIDE OF THE COVER PLATE SHALL BE WELDED AS FOLLOWS:
3/16" FILLET WELD BY 1" LONG SPACED AT 2".

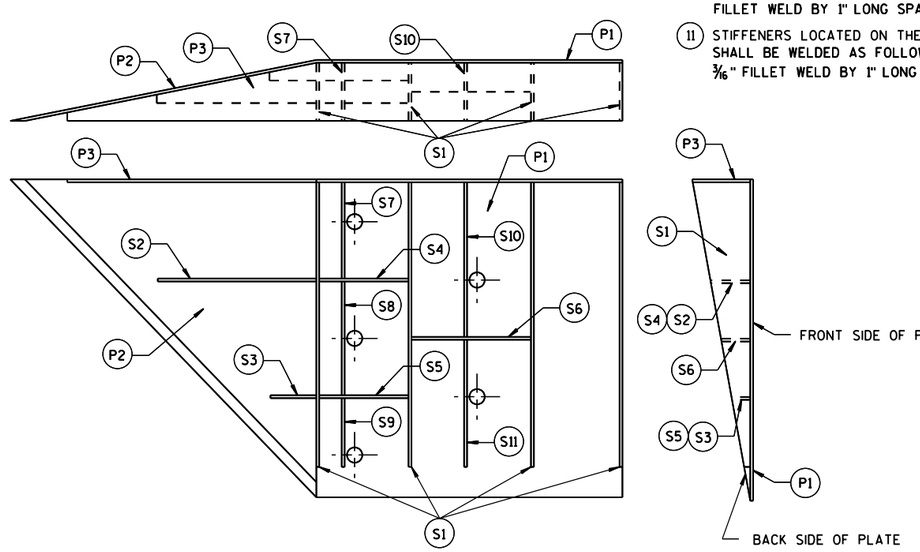


PLATE AND STIFFENER IDENTIFICATION

(VIEWED FROM BACK SIDE OF PLATE)

CONNECTOR PLATE DIMENSION (PER ASSEMBLY)

PLATE	QUANTITY	SHAPE	SIZE (A x B x C x D)	THICKNESS
P1	1		20" x 20"	3/16"
P2	1		20" x 20" x 28 9/16"	3/16"
P3	1		39" x 3 5/8" x 20" x 19 5/16"	3/16"
S1	4		18 7/16" x 3 3/8" x 18 3/4"	1/4"
S2	1		10 1/4" x 2 7/16" x 10 3/8" x 1/2"	1/4"
S3	1		3" x 1 1/16" x 3 1/8" x 1/2"	1/4"
S4	1		6 1/8" x 2 7/16"	1/4"
S5	1		6 1/8" x 1 1/16"	1/4"
S6	1		7 3/4" x 1 3/4"	1/4"
S7	1		2 9/16" x 6" x 3 5/8" x 5 7/8"	1/4"
S8	1		1 5/32" x 7 1/2" x 2 1/2" x 7 3/8"	1/4"
S9	1		6 1/16" x 6 3/16" x 1 1/32"	1/4"
S10	1		1 7/8" x 9 7/8" x 3 5/8" x 9 11/16"	1/4"
S11	1		8 1/2" x 8 3/4" x 1 13/16"	1/4"

SINGLE SLOPE CONNECTION PLATE

MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2015 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

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S.D.D. 14 B 45-4i

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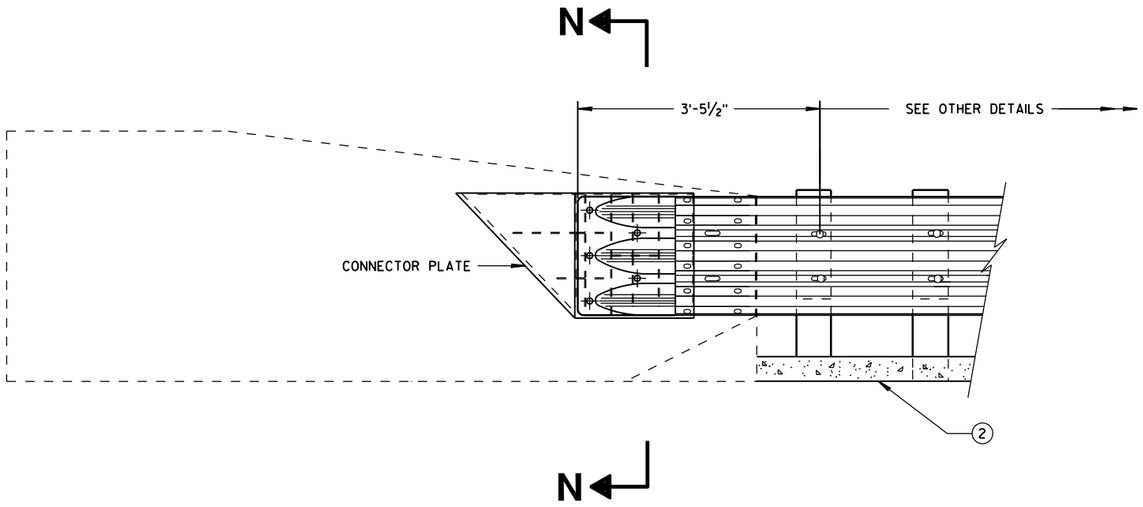
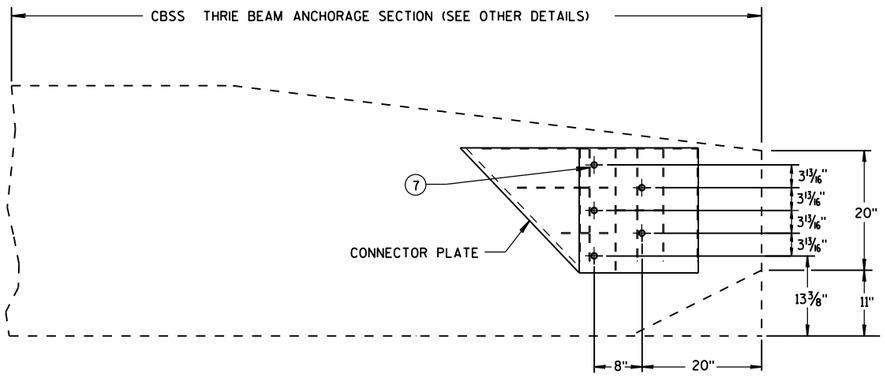
S.D.D. 14 B 45-4i



THRIE BEAM CONNECTION TO SINGLE SLOPE BARRIER



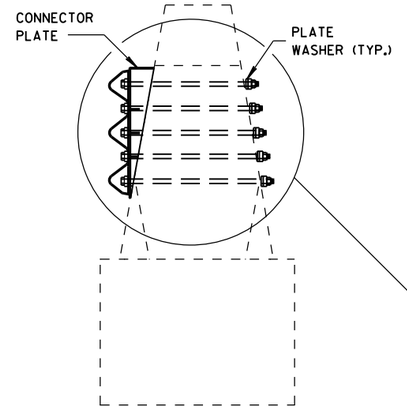
SINGLE SLOPE CONNECTION PLATE PLACEMENT

GENERAL NOTES

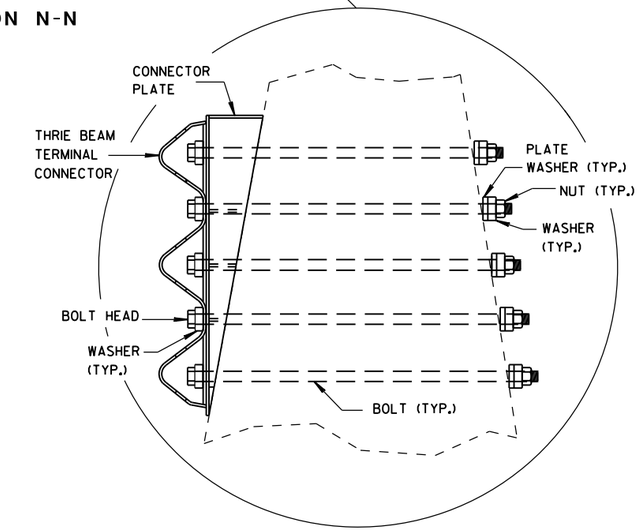
CONNECTOR PLATE, DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

(2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

(7) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/8" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.



SECTION N-N



MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2015 — DATE
/S/ Jerry H. Zoga — ROADWAY STANDARDS DEVELOPMENT ENGINEER
NT
FHWA

6

S.D.D. 14 B 45-4j

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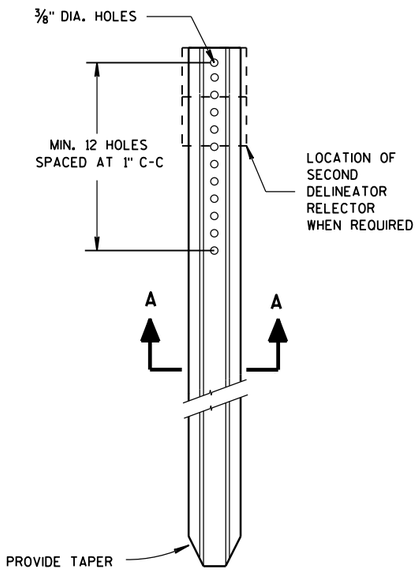
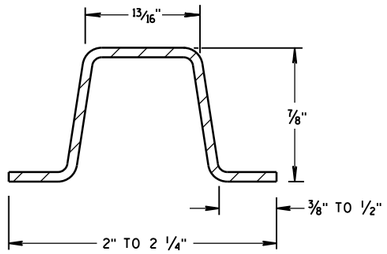
S.D.D. 14 B 45-4j

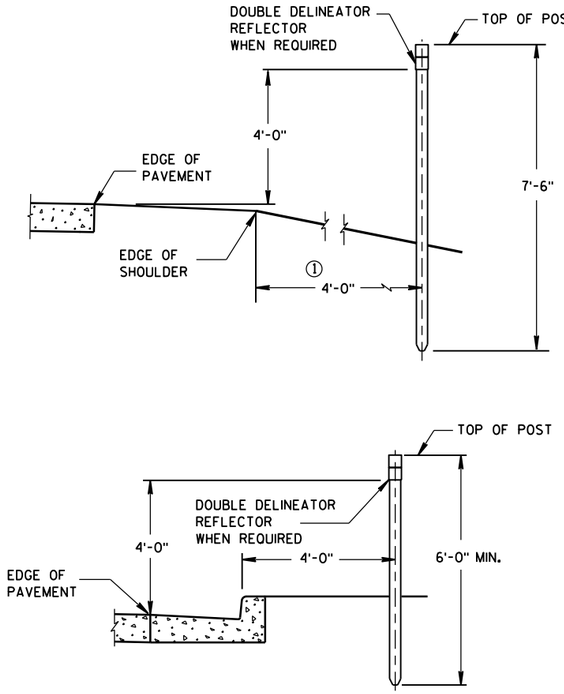


DELINEATOR POST



SECTION A-A

WEIGHT 1.12 LBS PER FT. ± 0.1 LB.

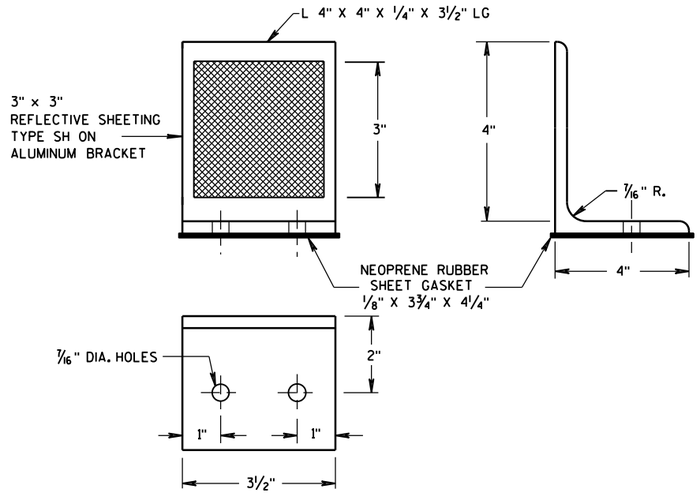


TYPICAL INSTALLATIONS OF DELINEATOR POSTS

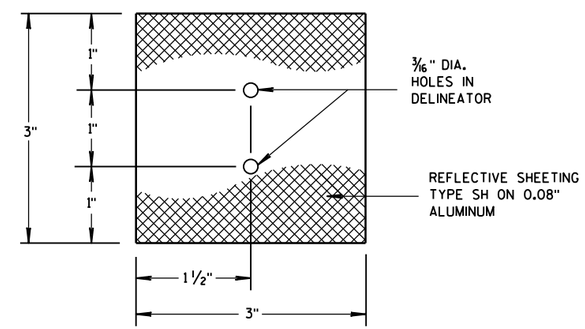
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

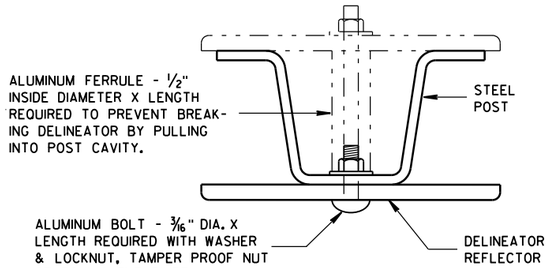
① DELINEATORS SHALL BE PLACED AT A CONSTANT DISTANCE FROM THE EDGE OF THE SHOULDER FOR THE LENGTH OF THE INSTALLATION.



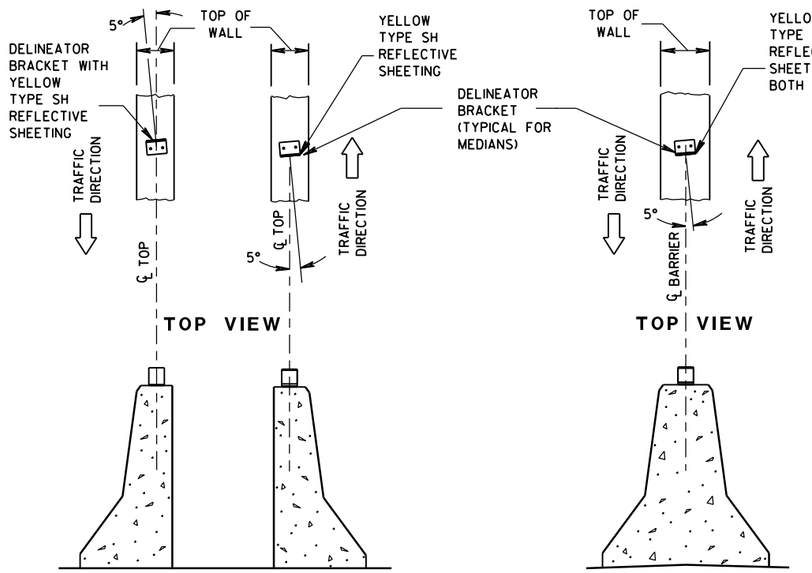
DELINEATOR BRACKET



3"x 3" DELINEATOR REFLECTOR

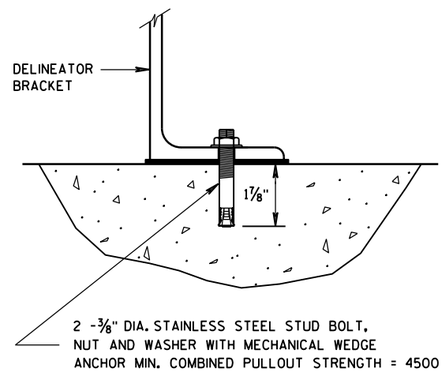


MOUNTING DETAIL FOR DELINEATOR REFLECTOR



DOUBLE BARRIERS IN MEDIAN | MEDIAN BARRIER | BARRIER LOCATED TO RT. OF TRAFFIC FLOW

LOCATION AND AIMING DETAILS FOR DELINEATOR BRACKETS MOUNTED ON CONCRETE BARRIERS



DELINEATOR BRACKET MOUNTING DETAIL

DELINEATOR POST, DELINEATOR REFLECTOR AND DELINEATOR BRACKET WITH REFLECTIVE SHEETING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

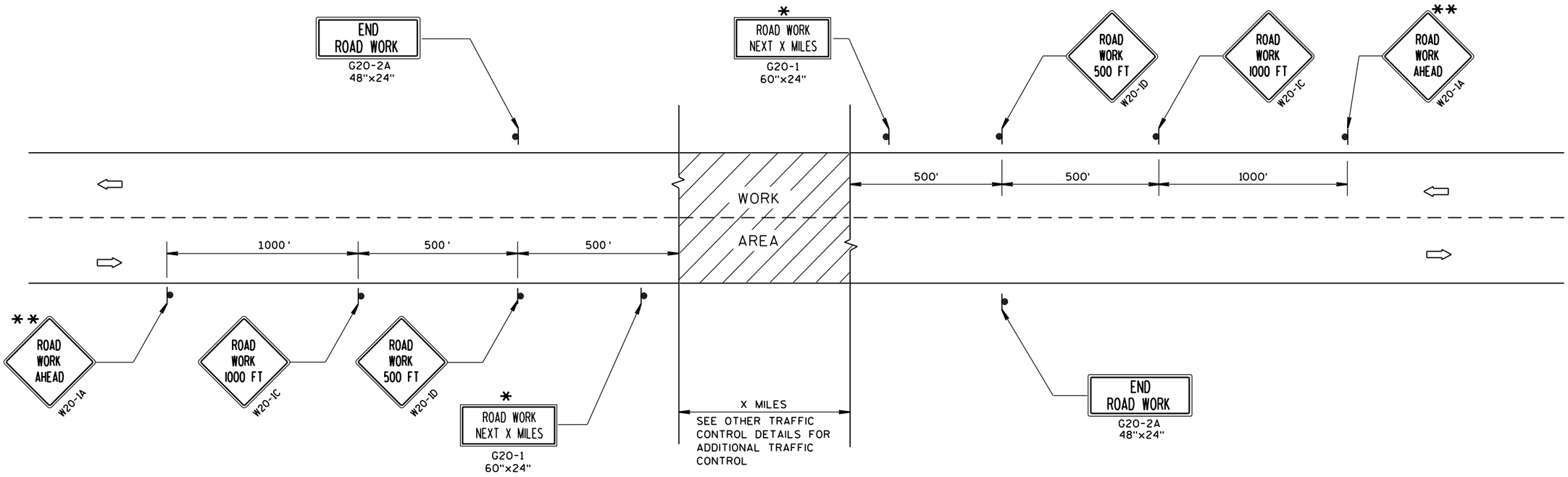
APPROVED
4-18-16 DATE
/S/ Matthew R. Rauch
STATE SIGNING AND MAR[illegible] ENGINEER
FHWA

6

S.D.D. 15 A 2-9

6

S.D.D. 15 A 2-9



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

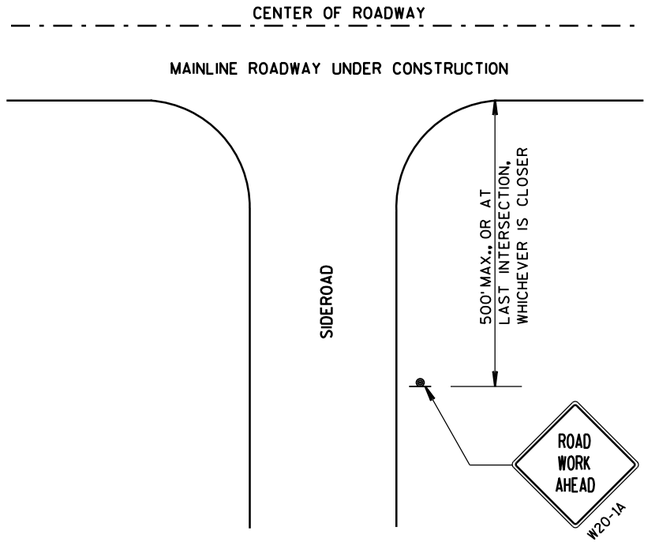
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

** PLACE ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

Sept. 2015 — DATE

/S/ Peter Amakobe Atepe — STATEWIDE WORK ZONE SAFETY ENGINEER

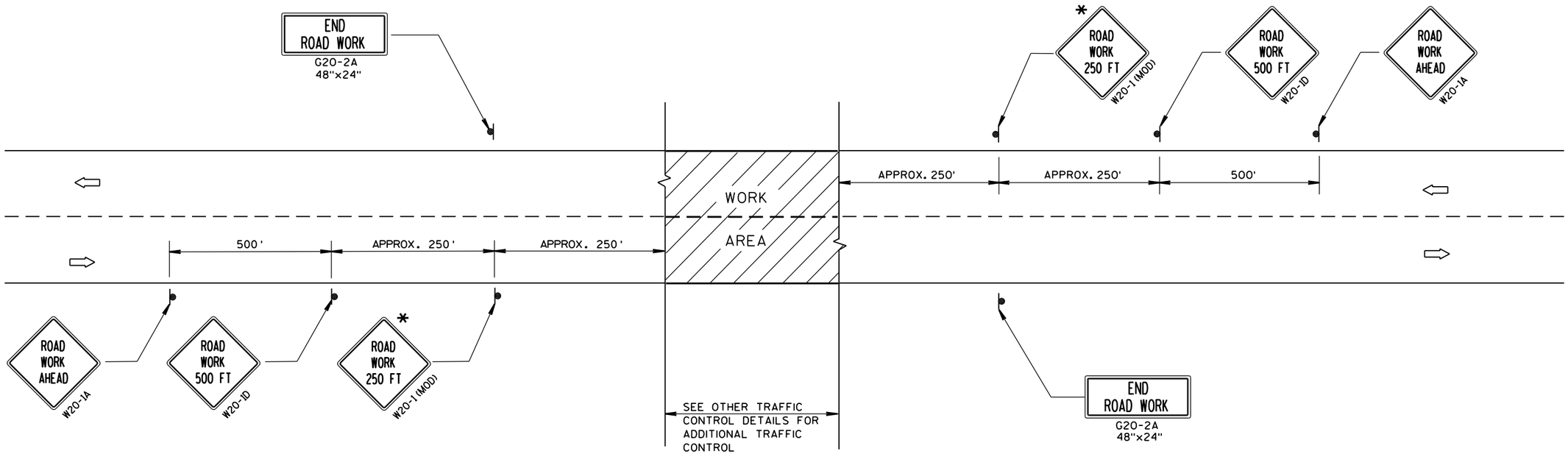
FHWA

6

S.D.D. 15 C 4-3

6

S.D.D. 15 C 4-3



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

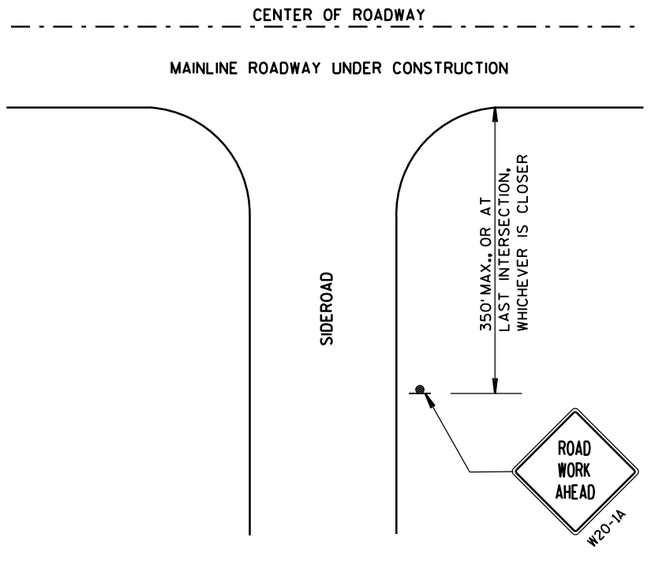
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

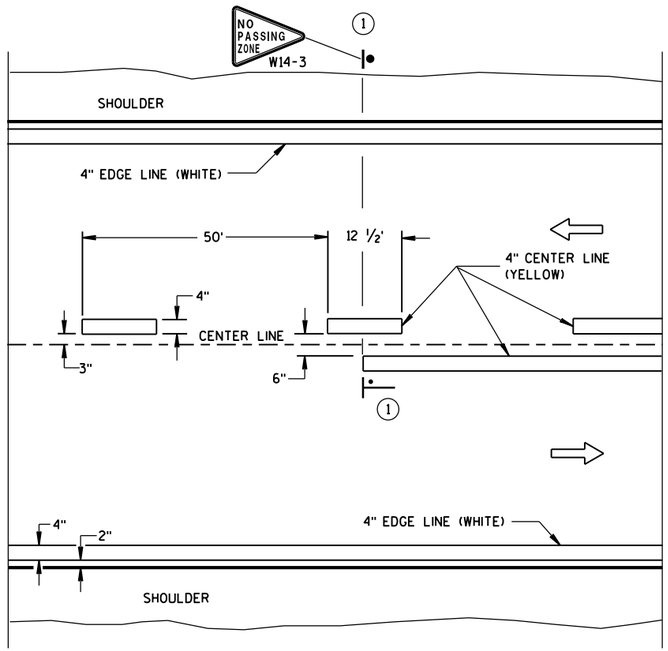
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

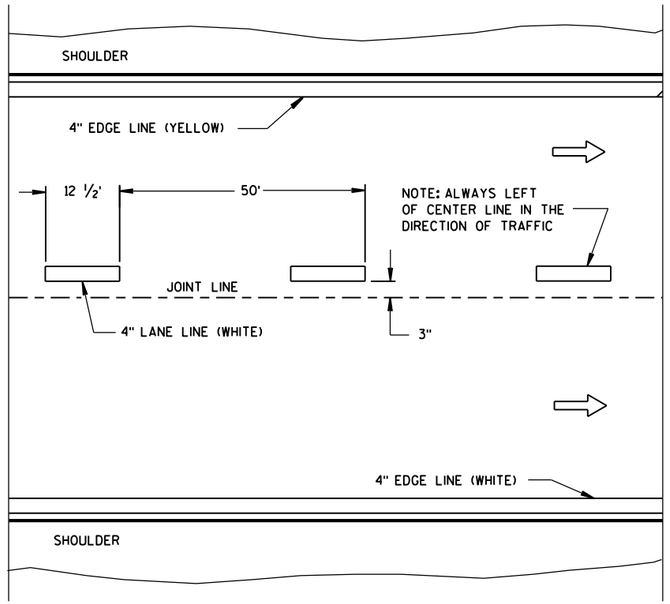
APPROVED
Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE SAFETY ENGINEER
FHWA

6

S.D.D. 15 C 5-3



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) NO PASSING ZONE W14-3 SIGN SHALL BE LOCATED WITHIN 50 FEET OF THE "T" MARKING.

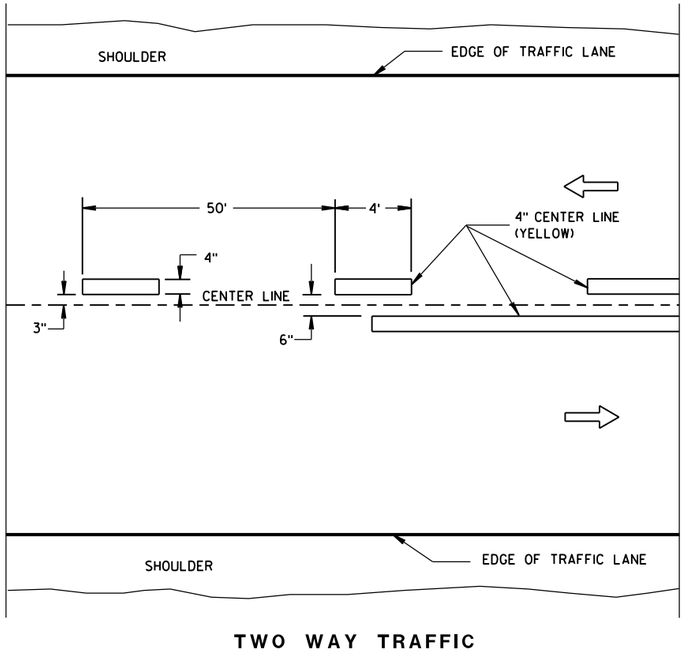
NOTE

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

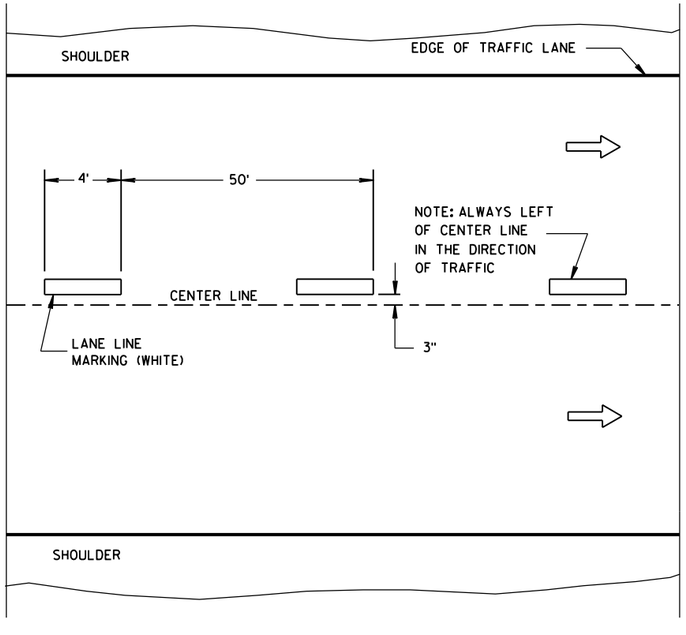
LEGEND

"T" MARKING

POST MOUNTED SIGN



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

LONGITUDINAL MARKING (MAINLINE)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
Sept., 2016	/S/ Matthew R. Rauch
DATE	STATE SIGNING AND MAR[illegible] ENGINEER
FHWA	

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S.D.D. 15 C 8-17a

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S.D.D. 15 C 8-17a

LEGEND

SIGN ON PORTABLE OR PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA

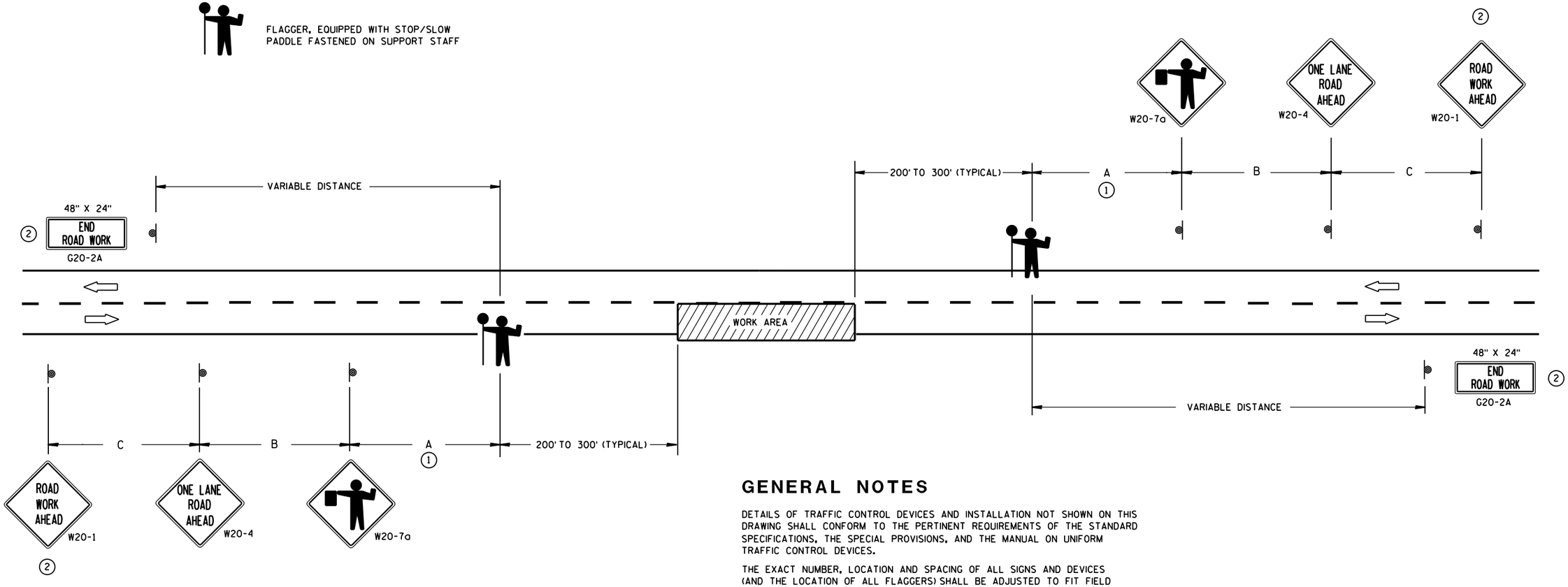
FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

W3-4

USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



(1) FOR A MOVING WORK OPERATION,
SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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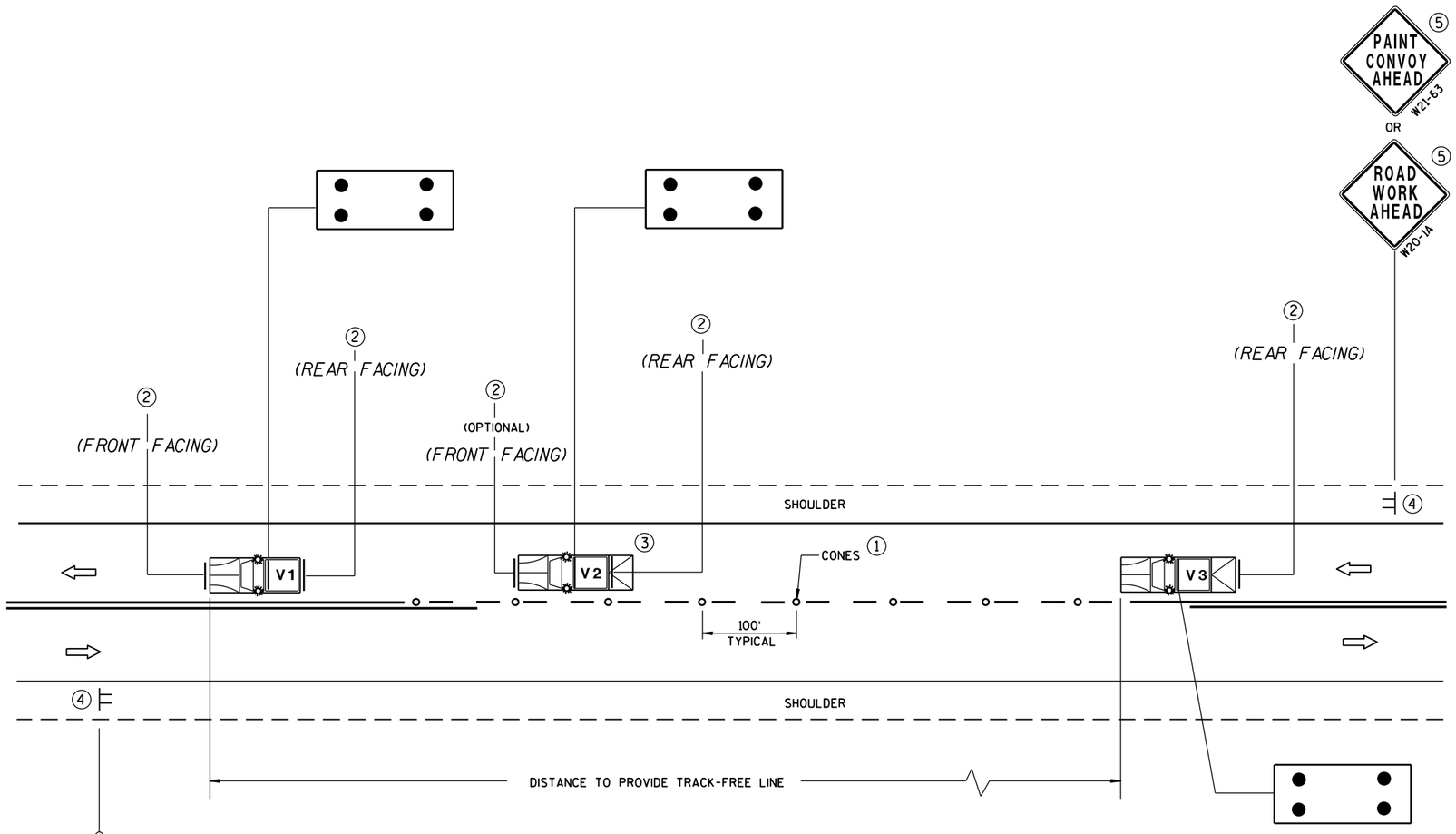
S.D.D. 15 C 12-4

6

S.D.D. 15 C 12-4



MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

(1) CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

(2) USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.

WET PAINT W21-64 OR WET PAINT W21-64

(3) OPTIONAL TRUCK-MOUNTED ATTENUATOR.

(4) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

(5) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

V1	LEAD VEHICLE
V2	SHADOW VEHICLE
V3	TRAIL VEHICLE WITH TMA
TMA	TRUCK-MOUNTED ATTENUATOR
	SIGN ON TEMPORARY SUPPORT
	DIRECTION OF TRAFFIC
	CONES
	FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION
APPROVED June 2016 — DATE /S/ Peter Amakobe Atepe — STATEWIDE WORK ZONE SAFETY ENGINEER FHWA

6

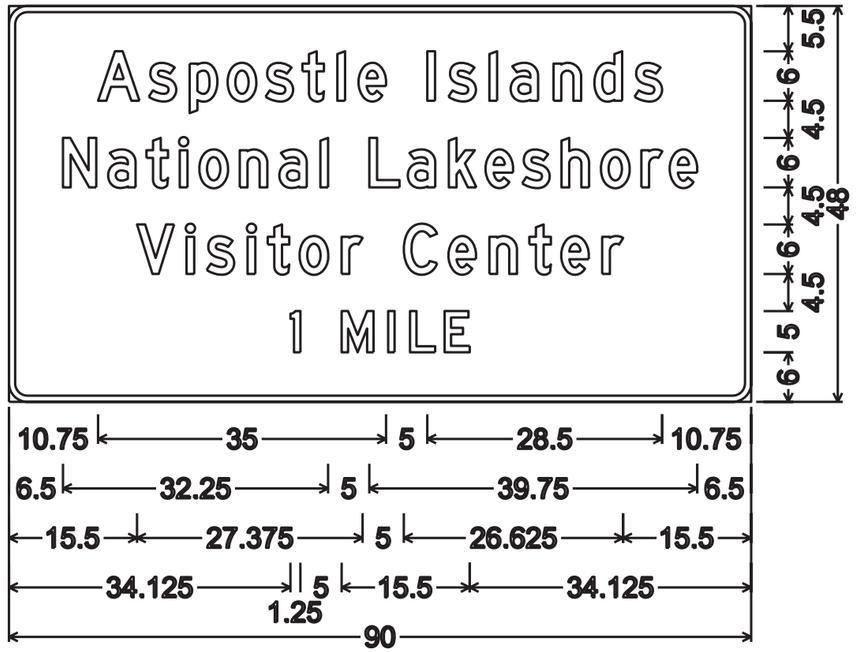
S.D.D. 15 C 19-4a

6

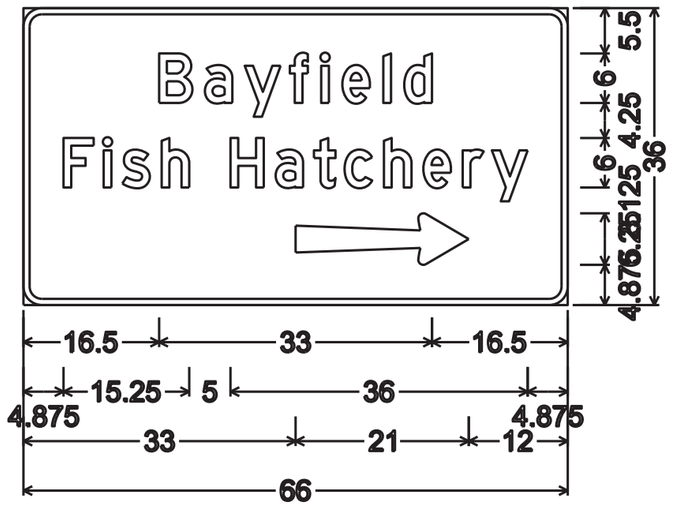
S.D.D. 15 C 19-4a

NOTES

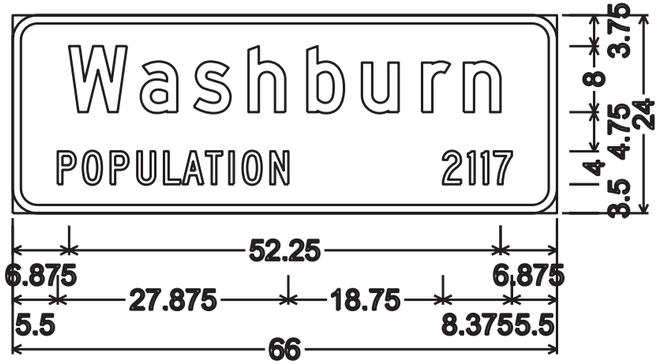
1. Signs are Type II - Type H Reflective
2. Color:
   Background - GREEN except as Shown
   Message - WHITE
3. Message Series - E except as Shown



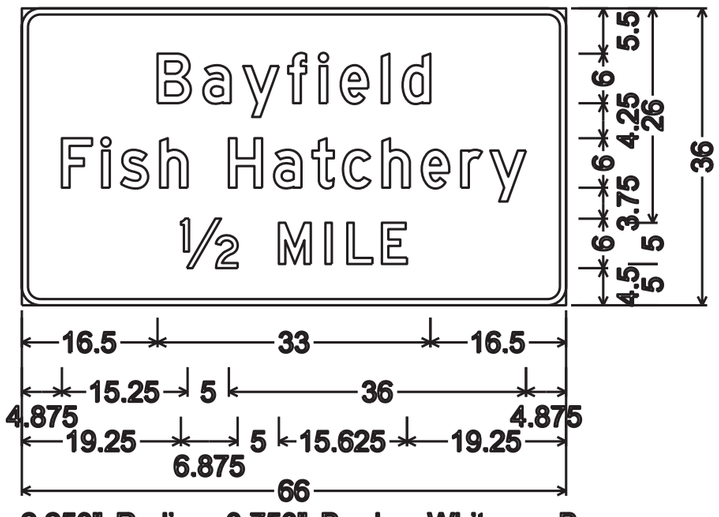
2.250" Radius, 0.750" Border, White on Brown;
"Aspostle" D; "Islands" D; "National" D;
"Lakeshore" D; "Visitor" D; "Center" D; "1" D; "MILE" D;



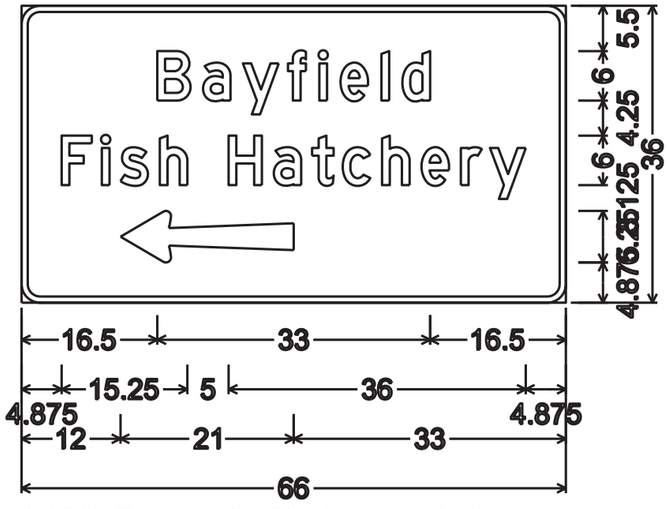
2.250" Radius, 0.750" Border, White on Brown;
"Bayfield" D; "Fish" D; "Hatchery" D



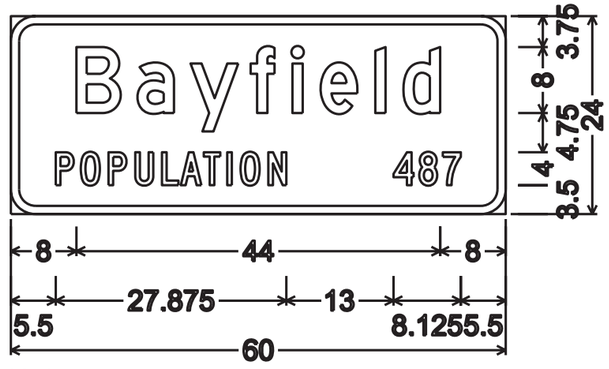
I2-3;
3.000" Radius, 1.000" Border,
"Washburn" D; "POPULATION" C; "2117" C



2.250" Radius, 0.750" Border, White on Brown;
"Bayfield" D; "Fish" D; "Hatchery" D; "½" D;
"MILE" D



2.250" Radius, 0.750" Border, White on Brown;
"Bayfield" D; "Fish" D; "Hatchery" D



I2-3;
3.000" Radius, 1.000" Border,
"Bayfield" D; "POPULATION" C; "487" C

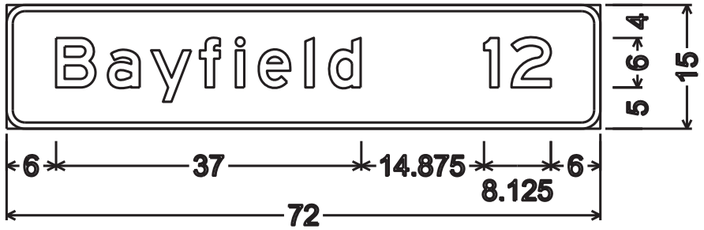
7

7

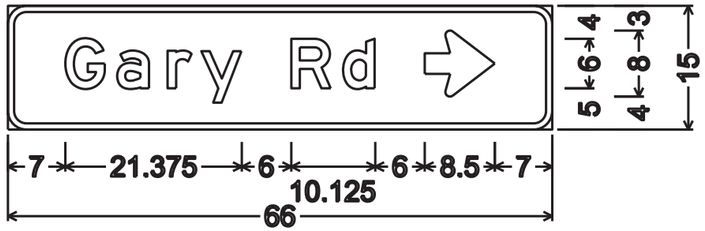
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET NO:	E

NOTES

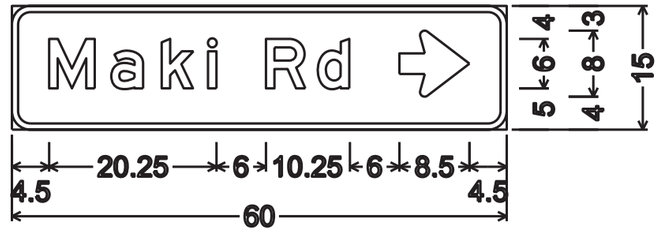
1. Signs are Type II - Type H Reflective
2. Color:
   Background - GREEN
   Message - WHITE
3. Message Series - E



D2-1;
2.250" Radius, 0.750" Border

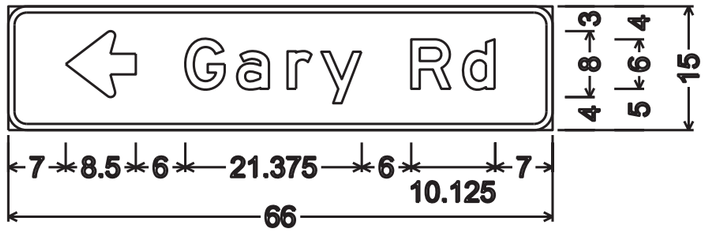


D1-1;
2.250" Radius, 0.750" Border



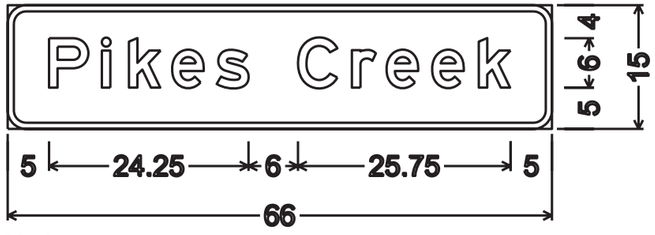
D1-1;
2.250" Radius, 0.750" Border

D2-2;
2.250" Radius, 0.750" Border



D1-1;
2.250" Radius, 0.750" Border

D1-1;
2.250" Radius, 0.750" Border



I3-1;
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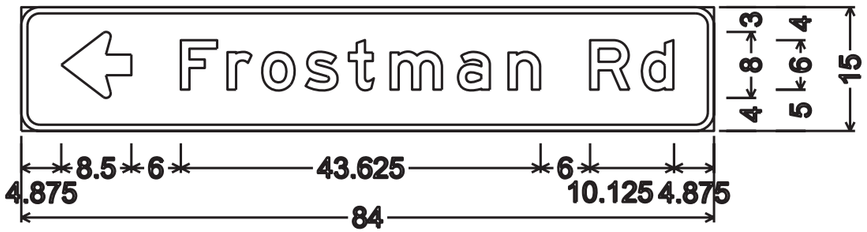
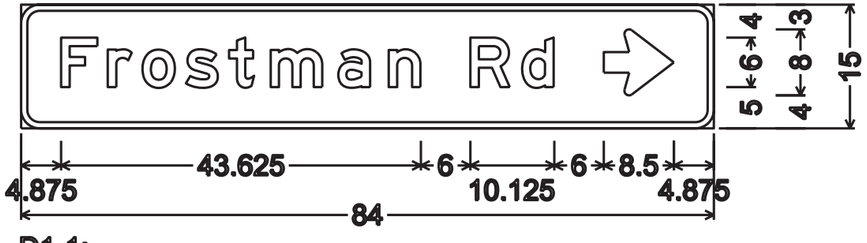
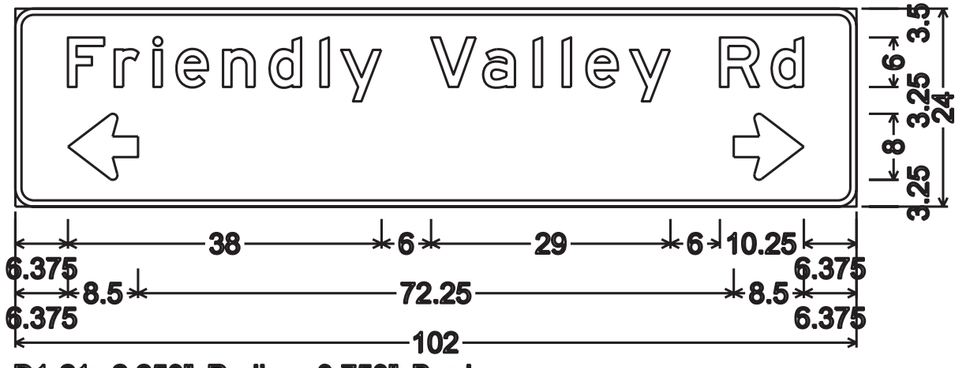


D1-1;
2.250" Radius, 0.750" Border



D1-1;
2.250" Radius, 0.750" Border

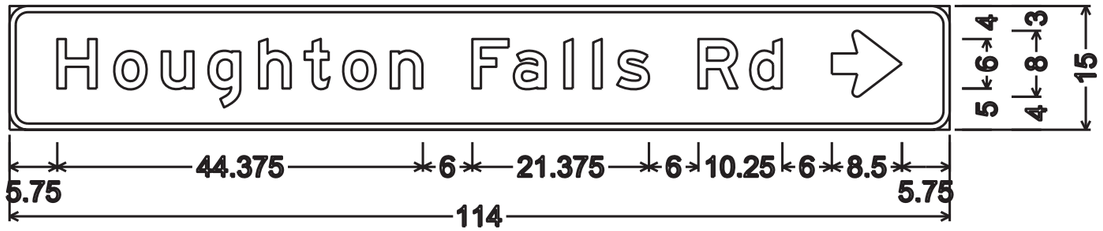


D1-61; 2.250" Radius, 0.750" Border

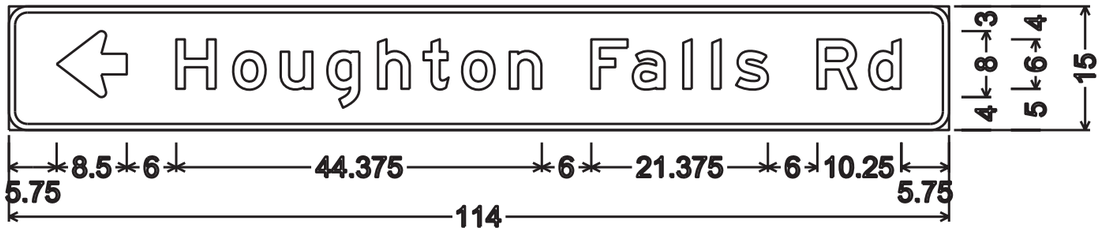
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET NO:	E

NOTES

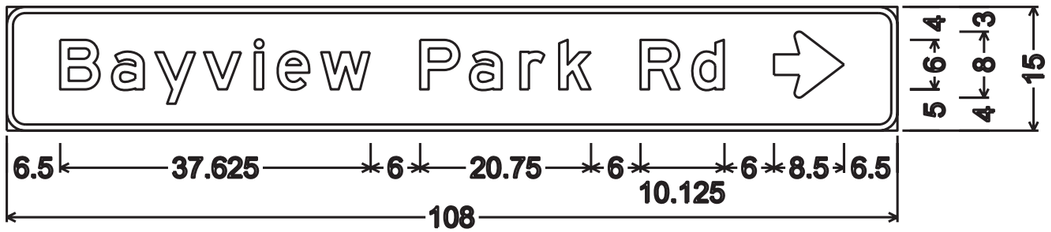
1. Signs are Type II - Type H Reflective
2. Color:
   Background - GREEN
   Message - WHITE
3. Message Series - E



D1-1; 2.250" Radius, 0.750" Border



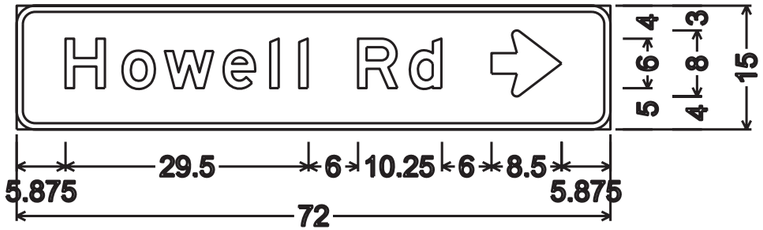
D1-1; 2.250" Radius, 0.750" Border



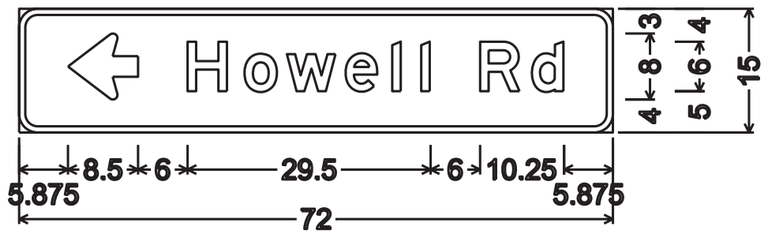
D1-1; 2.250" Radius, 0.750" Border

D1-1; 2.250" Radius, 0.750" Border

D1-1;
2.250" Radius, 0.750" Border



D1-1;
2.250" Radius, 0.750" Border



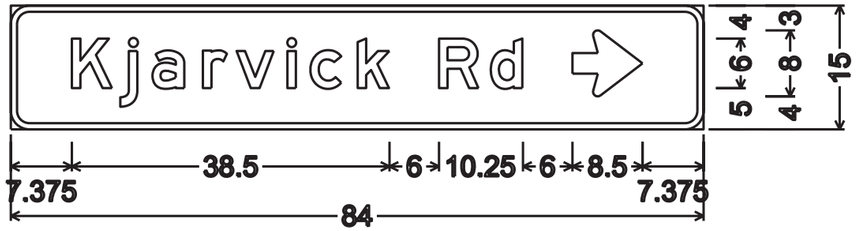
D1-1;
2.250" Radius, 0.750" Border

D1-1;
2.250" Radius, 0.750" Border

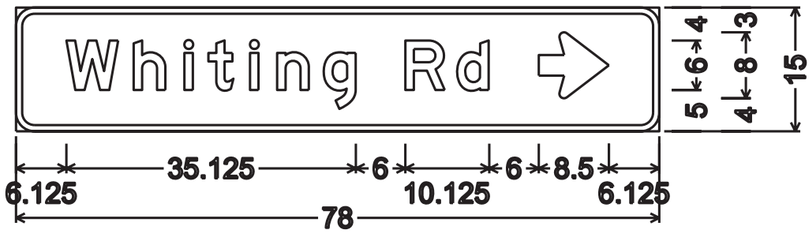
7

7

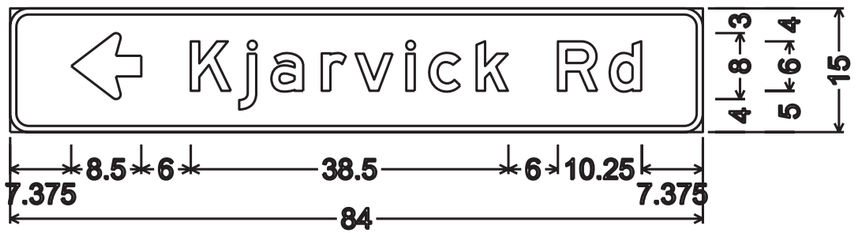
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET NO:	E



D1-1;
2.250" Radius, 0.750" Border

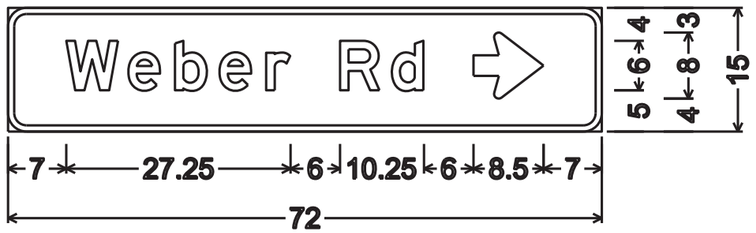


D1-1;
2.250" Radius, 0.750" Border

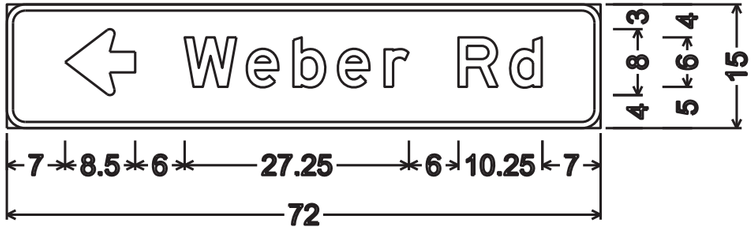


D1-1;
2.250" Radius, 0.750" Border

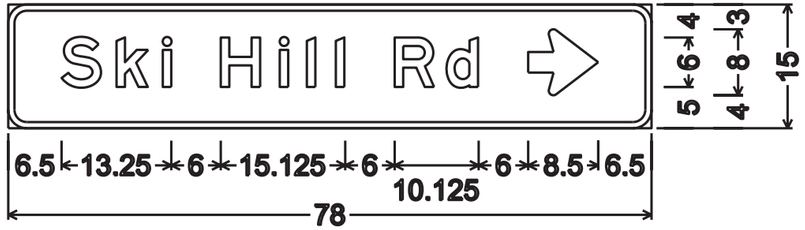
D1-1;
2.250" Radius, 0.750" Border



D1-1;
2.250" Radius, 0.750" Border



D1-1;
2.250" Radius, 0.750" Border



D1-1;
2.250" Radius, 0.750" Border

D1-1;
2.250" Radius, 0.750" Border

NOTES

1. Signs are Type II - Type H Reflective
2. Color:
   Background - GREEN
   Message - WHITE
3. Message Series - E

7

7

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET NO:	E

NOTES

1. Signs are Type II - Type H Reflective
2. Color:
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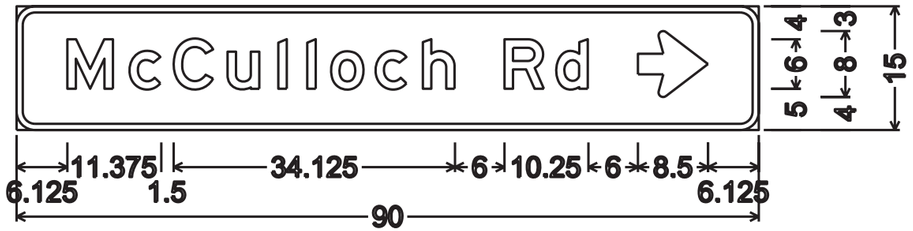
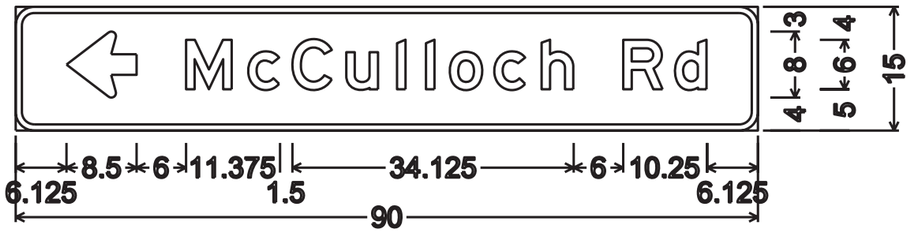
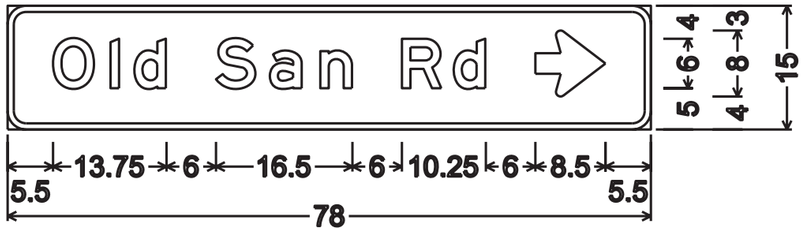
   Message - WHITE
3. Message Series - E



D1-1; 2.250" Radius, 0.750" Border

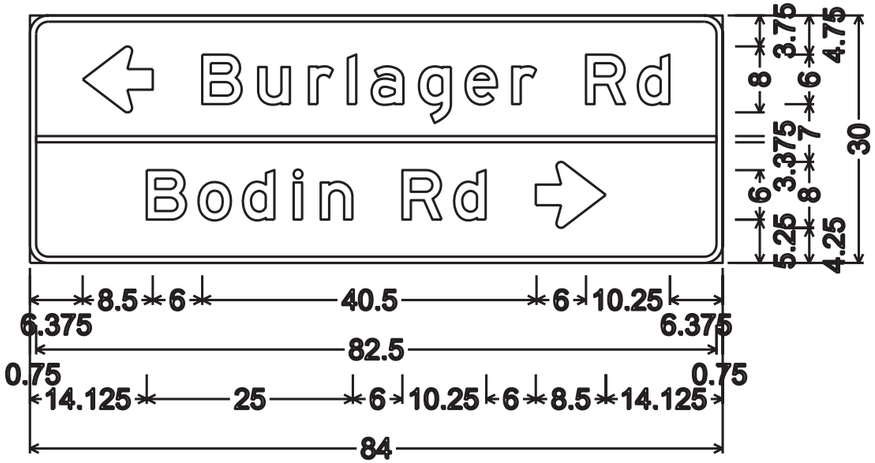


D1-1; 2.250" Radius, 0.750" Border

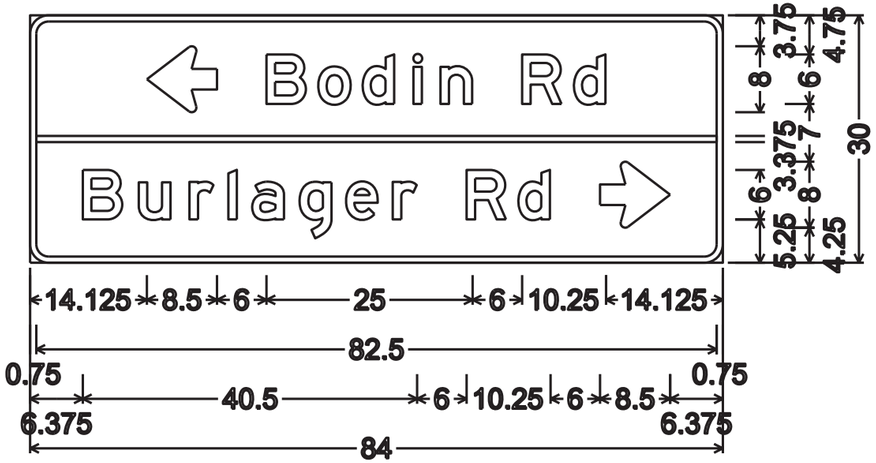


D1-1;
2.250" Radius, 0.750" Border

D1-1;
2.250" Radius, 0.750" Border



D1-2;
2.250" Radius, 0.750" Border



D1-2;
2.250" Radius, 0.750" Border

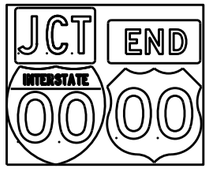
7

7

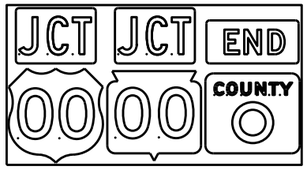
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	PERMANENT SIGNING	SHEET NO:	E

TYPICAL ASSEMBLIES

J1-1

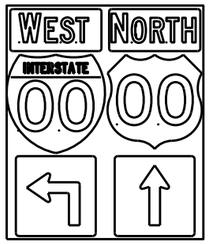


J1-2

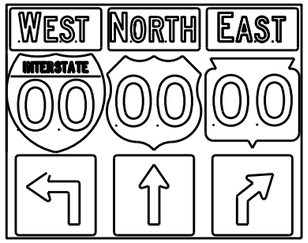


J1-3

J2-1



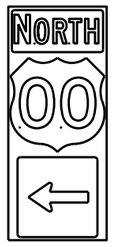
J2-2



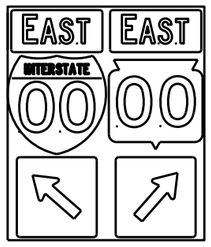
J2-3

JV

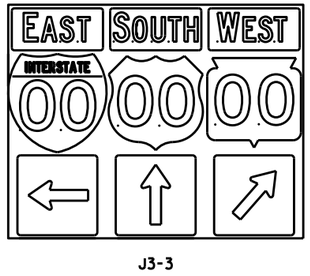
(Typical Vertical J-Assembly
See Note 10 and 11)



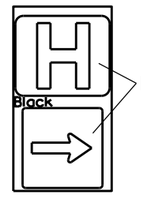
J3-1



J3-2

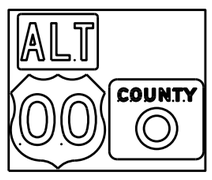


J3-3

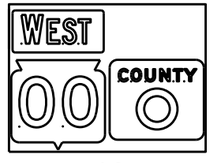


JH-1

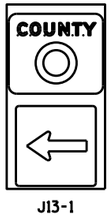
J4-1



J4-2



J4-2



J13-1

J12-1

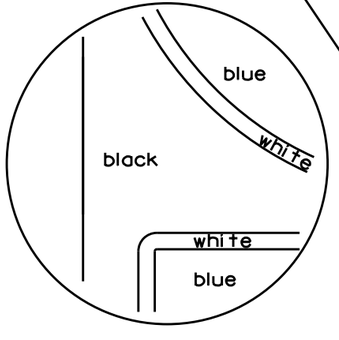
J32-1

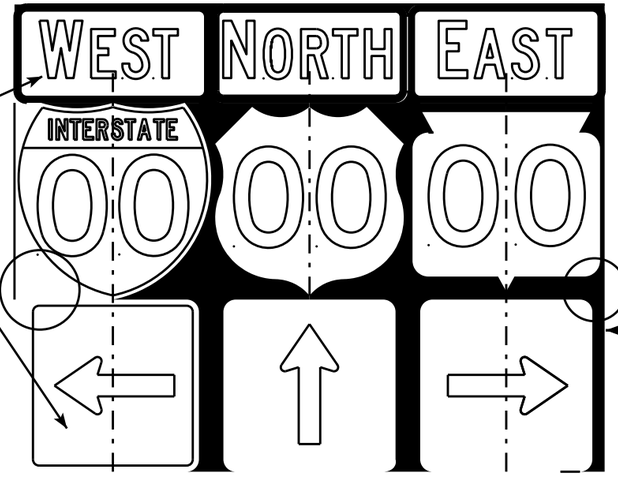
J33-1

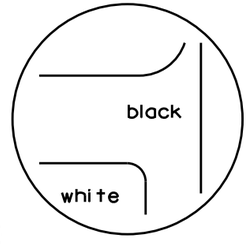
J23-1

J22-1

[blue background with interstate]







[black background]

NOTES

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Black Non-reflective
   Message - see Note 5
3. Message Series - See Note 5
4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inchs (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
10. All Vertical J Assemblies are given a Sign Code of JV
11. For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

ROUTE MARKERS & COMPONENTS
IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

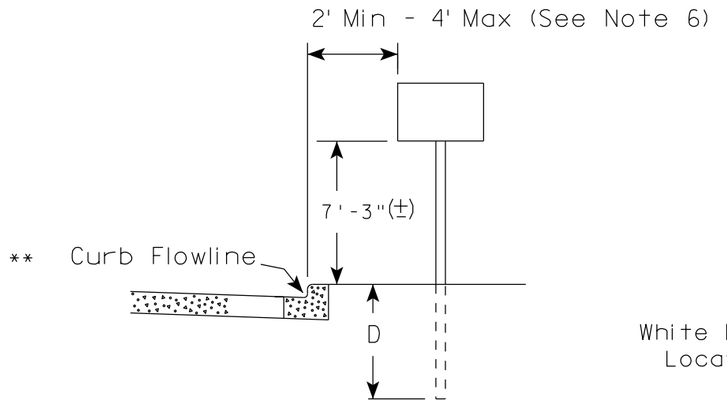
DATE 2/06/14 PL... A2-1S.8

PROJECT NO: SHEET NO: E

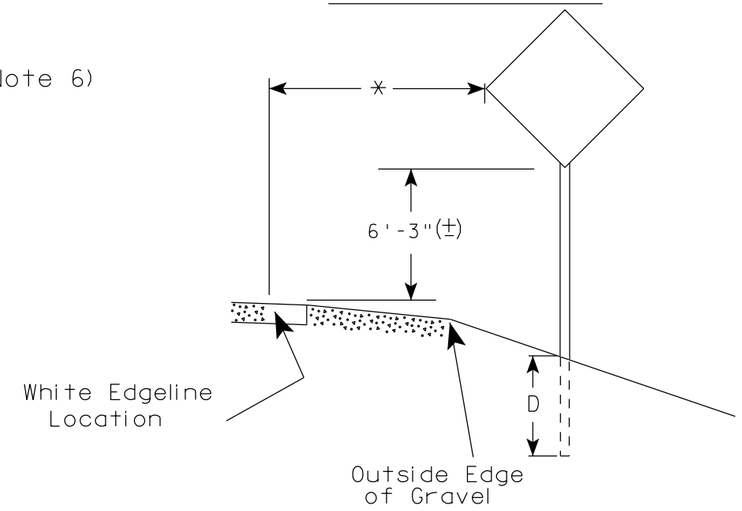
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A21S.DGN PLOT DATE : 06-FEB-2014 14:10 PLOT BY : mscsja PLOT NAME :

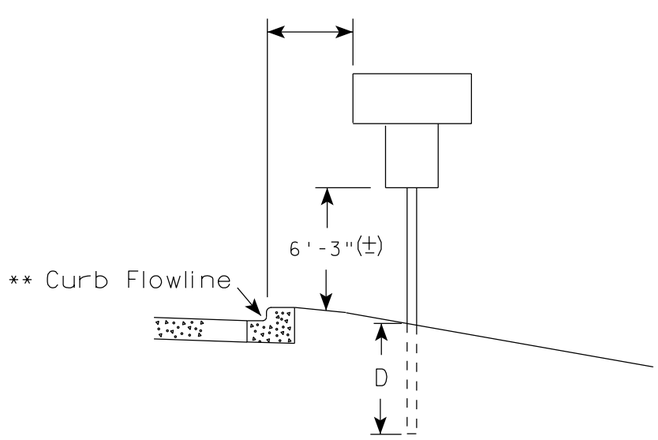
WISDOT/CADDS SHEET 42

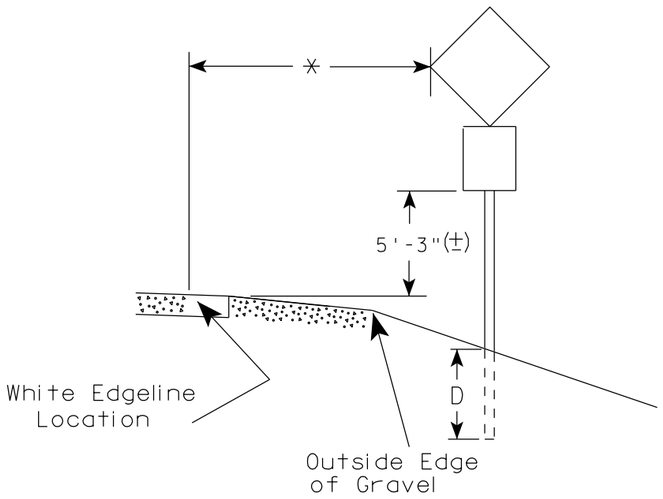
URBAN AREA



RURAL AREA (See Note 2)







GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

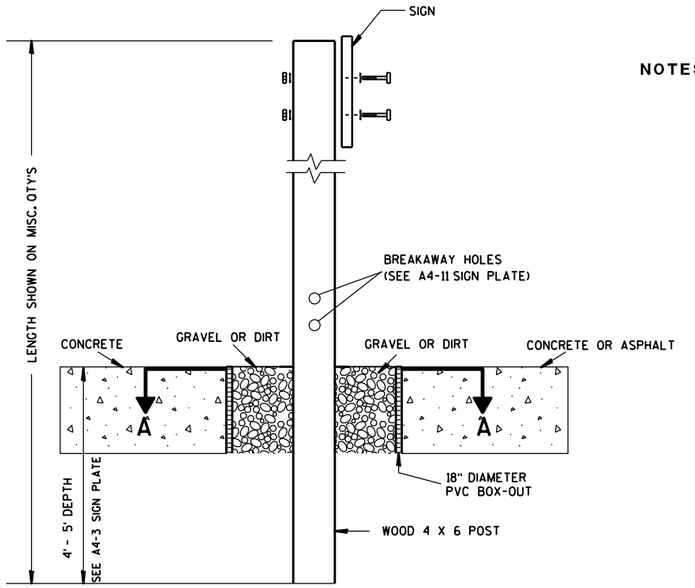
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-3.20

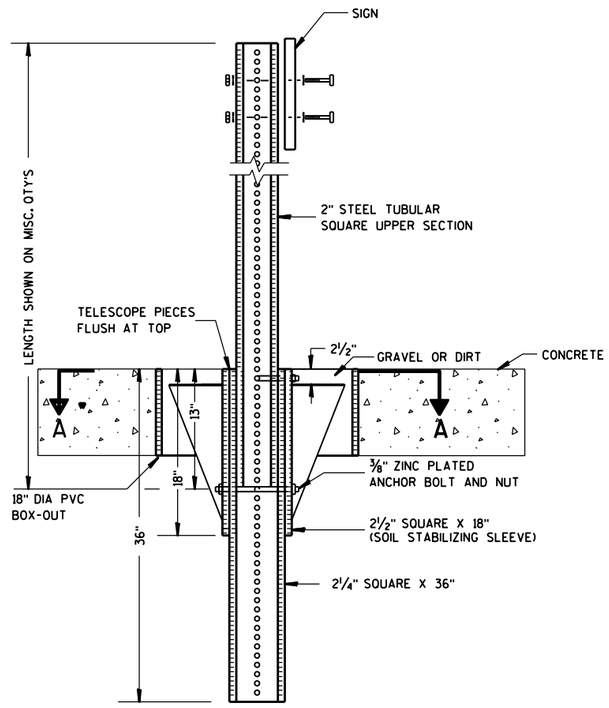
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



ELEVATION VIEW

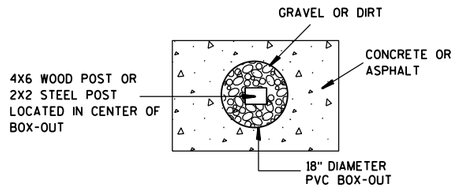
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

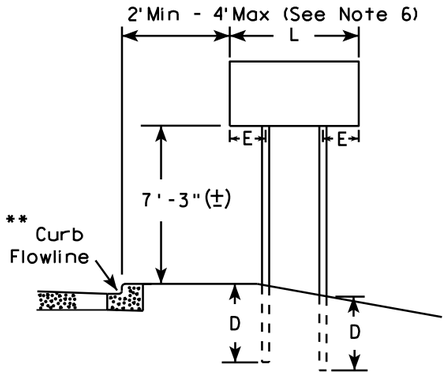
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

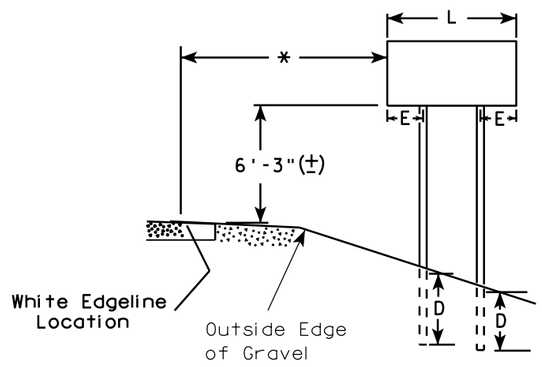
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

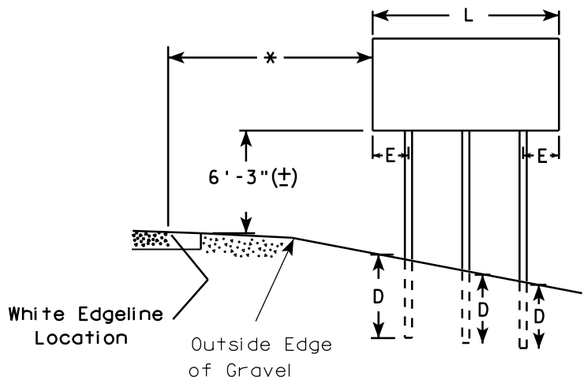
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

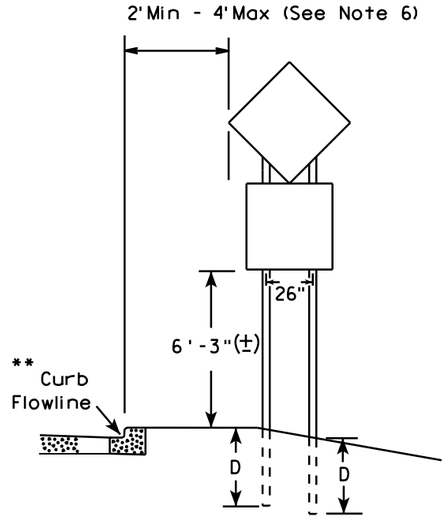
URBAN AREA



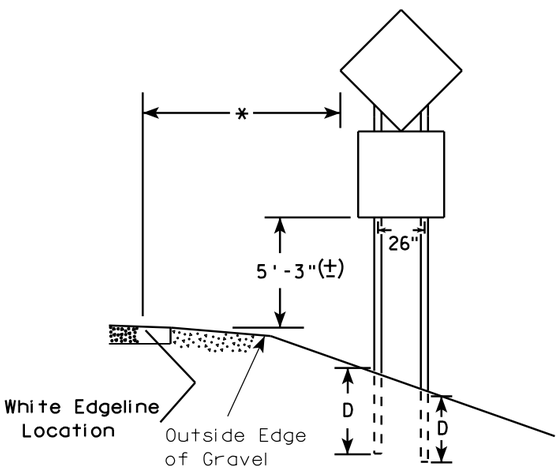
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

***

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

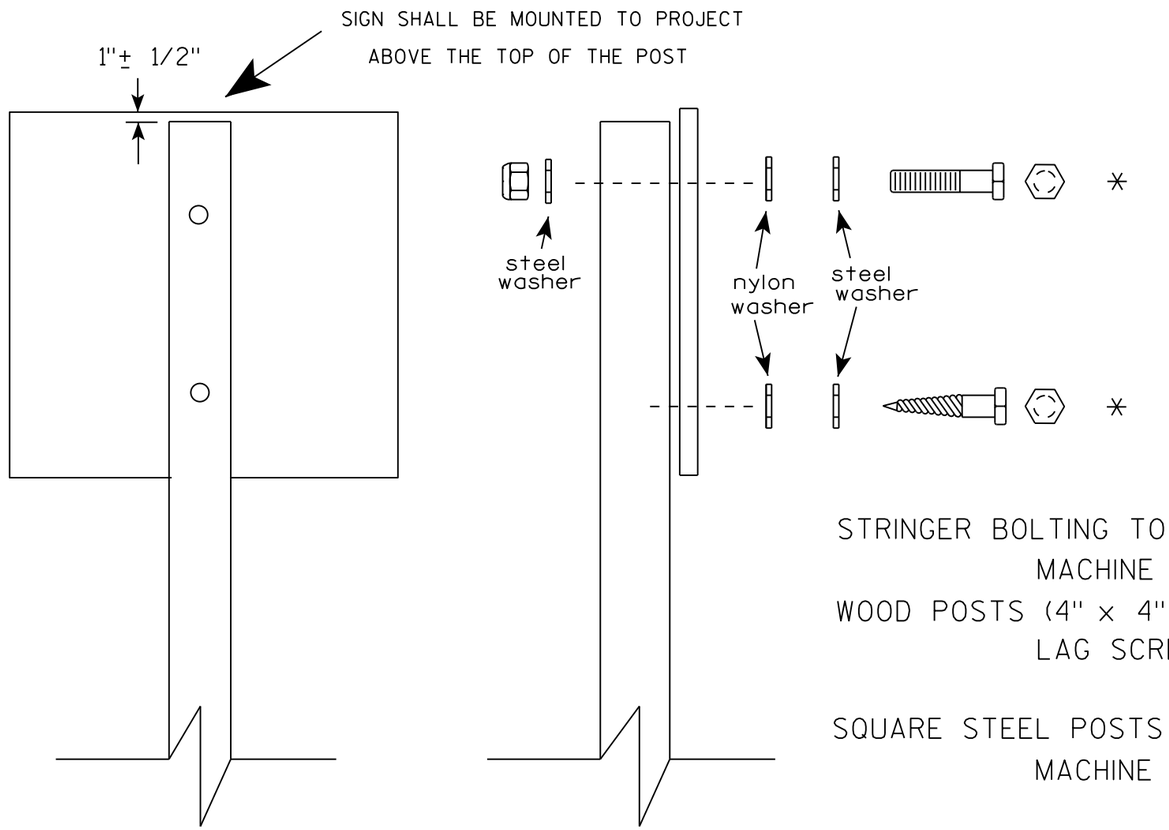
DATE 7/23/15 PLATE NO. A4-4 14

107

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A44.DGN PLOT DATE : 23-JUL-2015 15:23 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 107.021305:1.000000

WISDOT/CADDS SHEET 42



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 4" or 4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)

3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)

3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/11/16 PLATE NO. A4-8.8

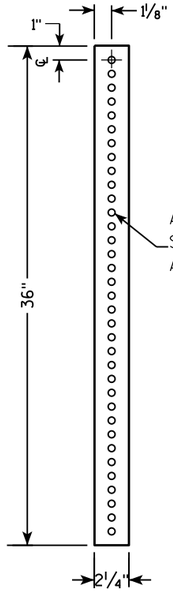
7

PROJECT NO: SHEET NO: E

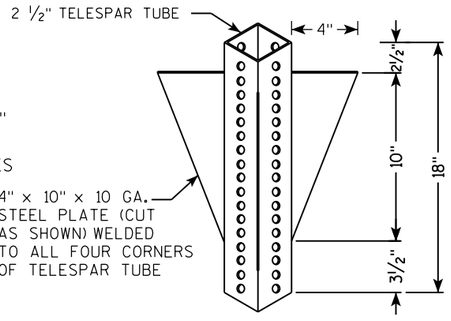
FILE NAME : C:\CADfiles\Projects\tr_stdplate\A48.DGN PLOT DATE : 11-AUG-2016 11:35 PLOT BY : $$ plotuser $$

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

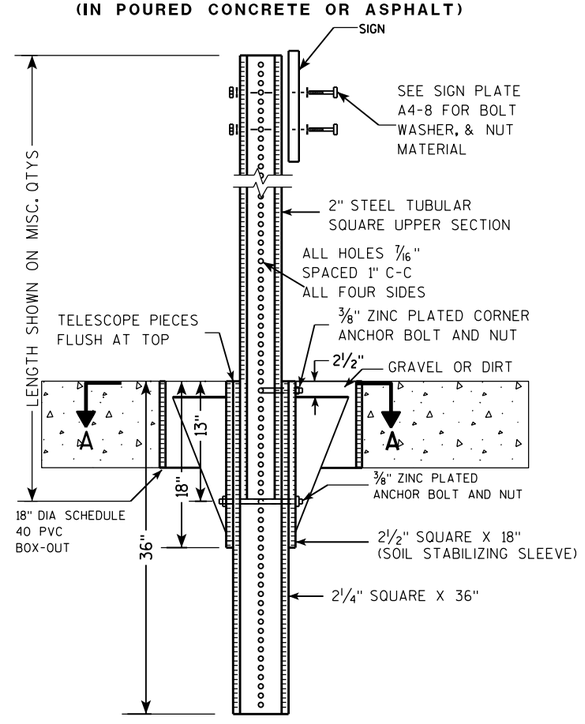
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



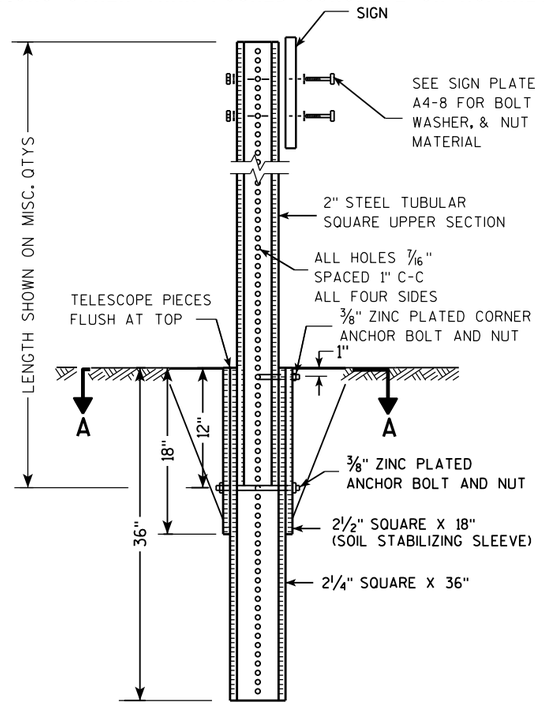
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

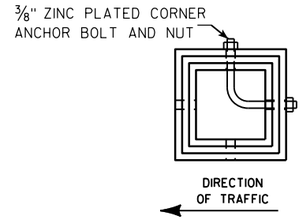


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

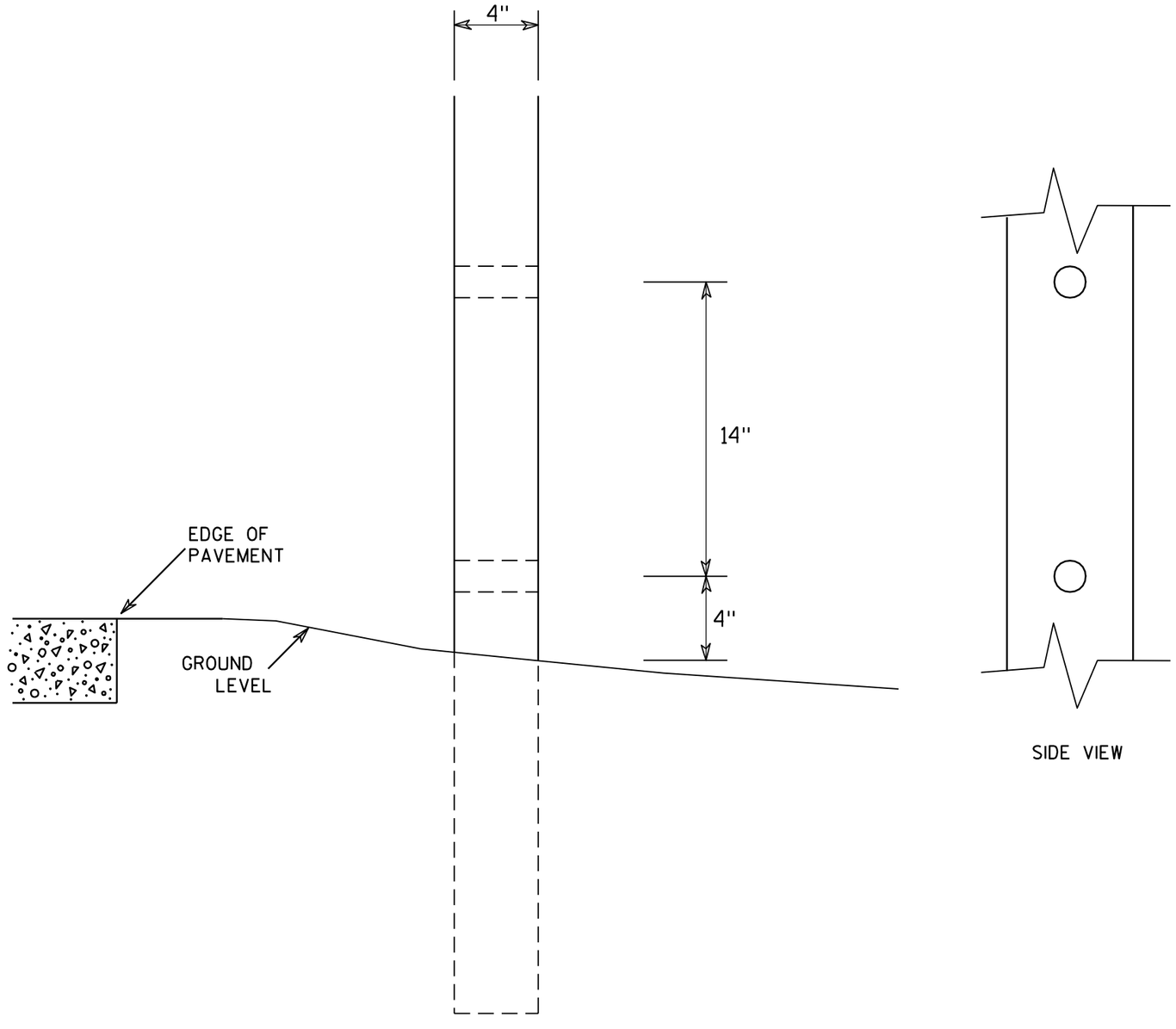
TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

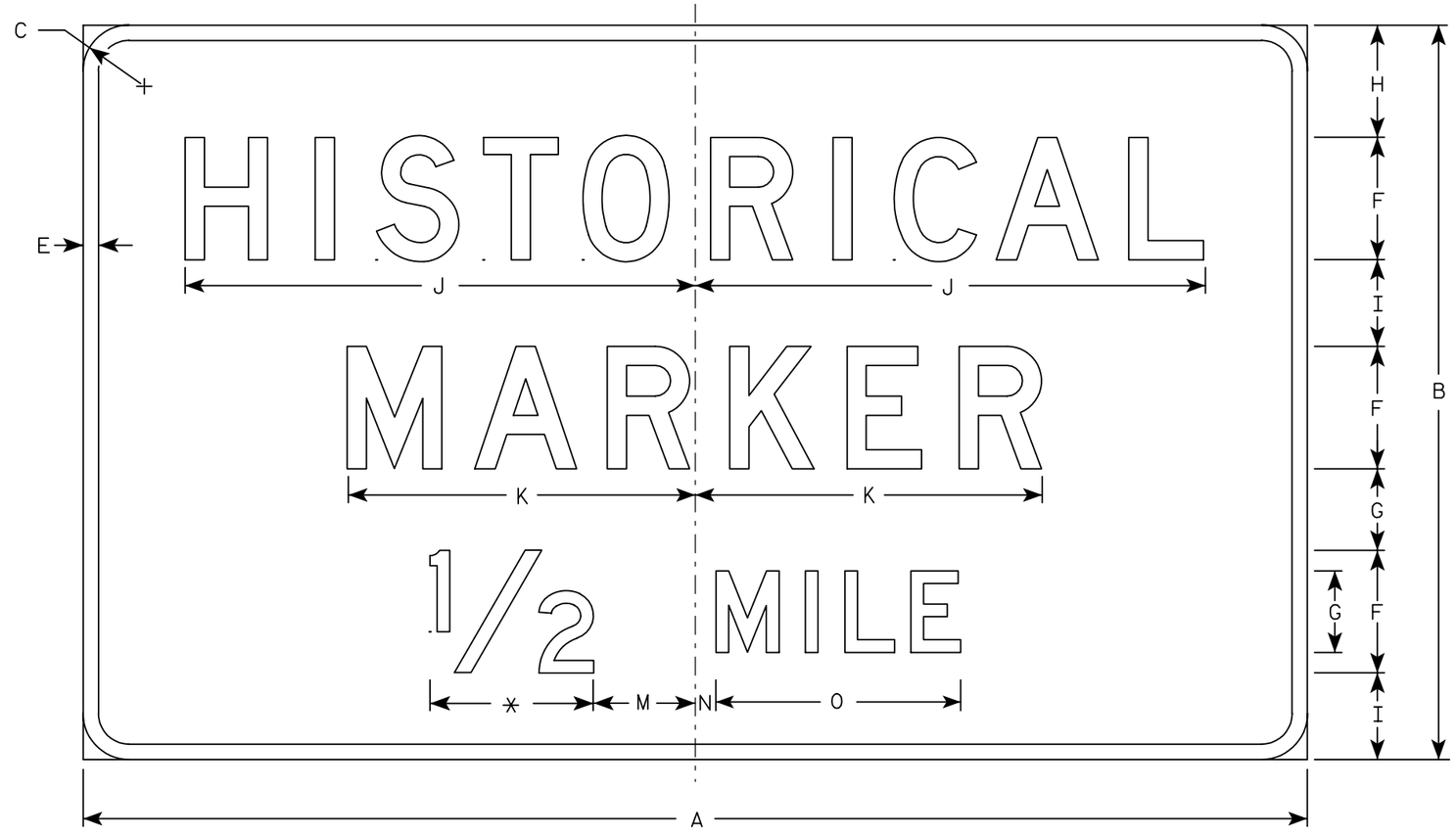
4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Chester J Spang for State Traffic Engineer

DATE 3/27/97 PLATE NO. A4-11.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 110	E



D5-63 * See Note 5

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Brown
   Message - White
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

Metric equivalent for this sign is:

SIZE	
1	
2	1500 mm X 900 mm
3	
4	
5	

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	60	36	2 1/4		3/4	6	4	5 1/2	4 1/4	25	17		5	1	12												15.0	1.35
3																												
4																												
5																												

STANDARD SIGN
D5-63

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Chester J Spang for State Traffic Engineer

DATE 3/23/99 PLATE NO. D5-63.9

STATE PROJECT NUMBER:

SHEET NO: 111 E

7

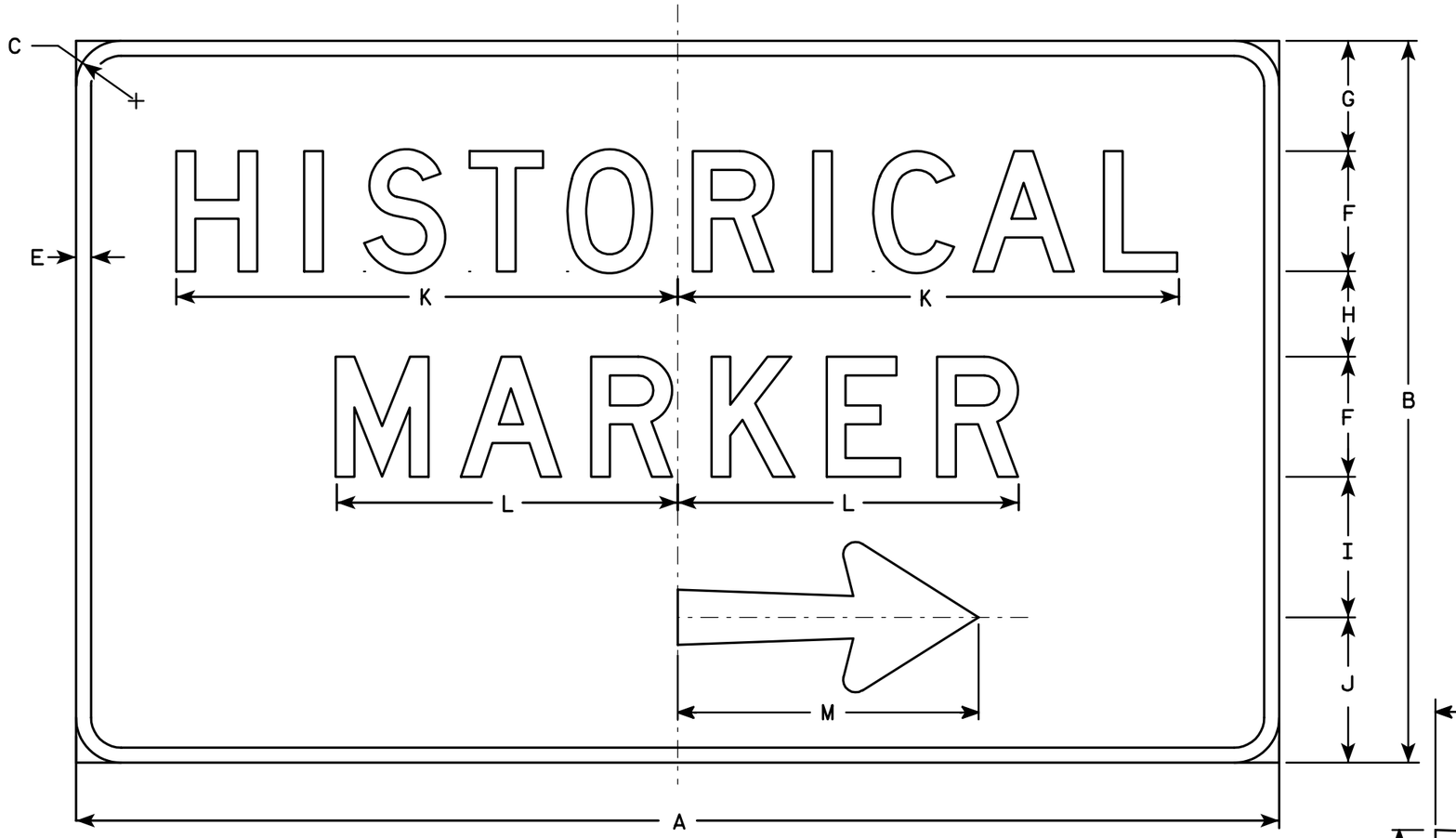
7

58.59.60.61.62.63

LEVELS ON - 2.3. 5.6. 10.

FILE NAME : C:\Users\Projects\tr_stdplate\D563.DGN PLOT DATE : 09-JAN-2002 13:18 ORG DATE : 3/23/99 Originator : Don Kluever

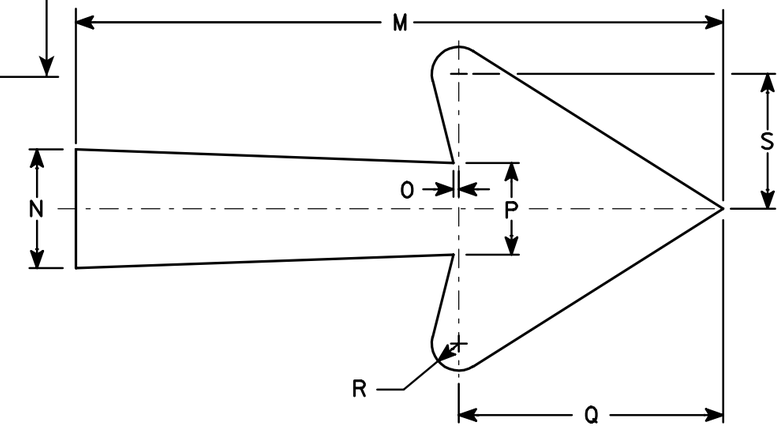
WISDOT/CADDS SHEET 42



D5-64R

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Brown
   Message - White
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. D5-64L is the same as D5-64R except the arrow is reversed.



Arrow Detail

Metric equivalent for this sign is:

SIZE	
1	
2	1500 mm X 900 mm
3	
4	
5	

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	60	36	2 1/4		3/4	6	5 1/2	4 1/4	7	7 1/4	25	17	15	2 3/4	1/8	2 1/8	6 1/8	5/8	3 1/8								15.0	1.35
3																												
4																												
5																												

STANDARD SIGN
D5-64

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Charles J Spay
for State Traffic Engineer

DATE 1/09/02 PLATE NO. D5-64.5

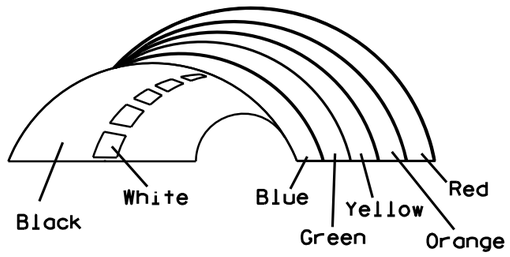
STATE PROJECT NUMBER:

SHEET NO: 112 E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - (See Note 5)
3. Message Series - (See Note 6)
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Border - Blue
   Line 1 - Red
   Line 2 - Black
   Line 3-5 - Blue
6. Line 1 - Dutch 8011L
   Line 2 - Series E
   Line 3-5 - Series C
7. Contractor shall provide and install a new post bracket in accordance with the I55-56B sign detail.

Background Colors of Symbol*



*1/4" Black Border between each color of rainbow and border of rainbow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	36	1 1/2	1/2	5/8	3	2	3 1/2	2 7/8	1	8	2 1/8	11 1/4	11 1/8	9 3/8	1 1/4		3/4	12 5/8	7 1/2							7.5
3																											
4																											
5																											

STANDARD SIGN
I55-56

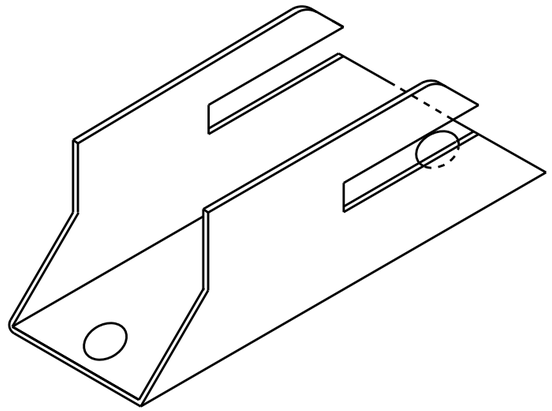
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

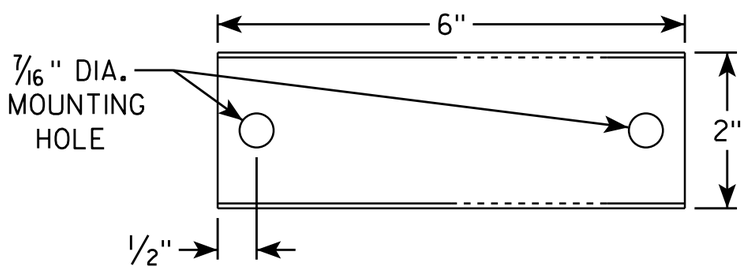
DATE 4/27/11 PLATE NO. 155-56.3

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 113

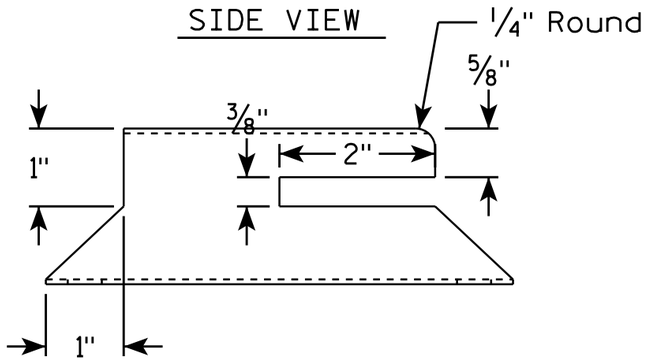
ISOMETRIC VIEW



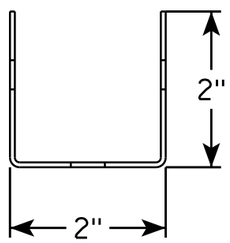
TOP VIEW



SIDE VIEW



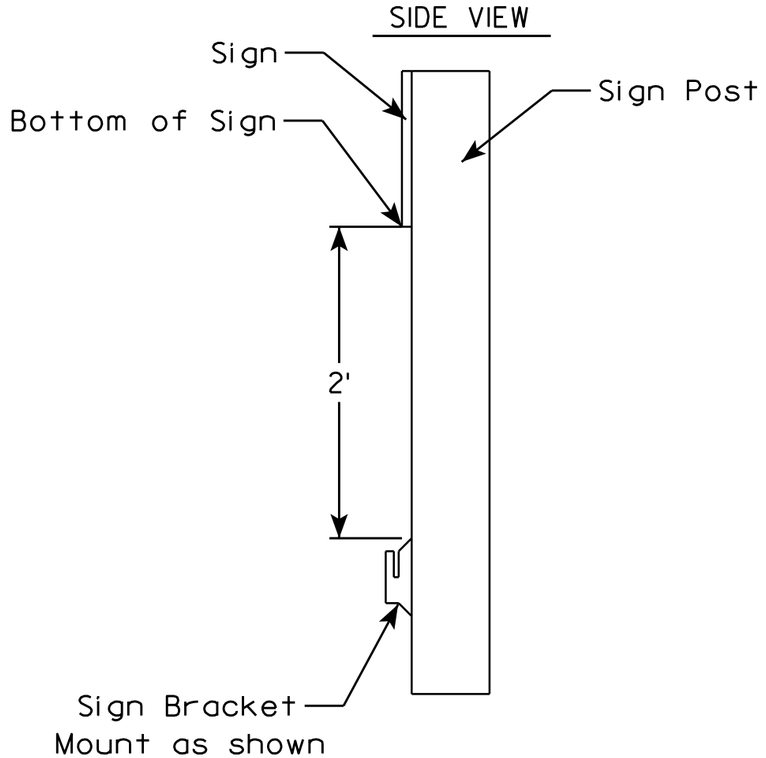
END VIEW



NOTES

1. Must be capable of permanent attachment to a wood or steel channel sign post utilizing the fastening hardware specified on the A4-8 sign plate.
2. Shall be entirely primed and painted with two coats of a black powder coated enamel paint.
3. Shall be made with 12 gauge steel, and incorporate no welds, no hinged components, no threaded lock-type components, and no parts which are loose or can be separated from the main body.
4. Shall have rounded edges with at least 1/8" radii.
5. Shall not have unrounded and uncoated metal edges which can contact the back surface of the roll-up sign.
6. Top of bracket shall be mounted 2' below the bottom of the I55-56 sign.
7. Cost of bracket and fastening hardware shall be incidental to the I55-56 sign.

SIDE VIEW



ROLLUP SIGN BRACKET
I55-56B

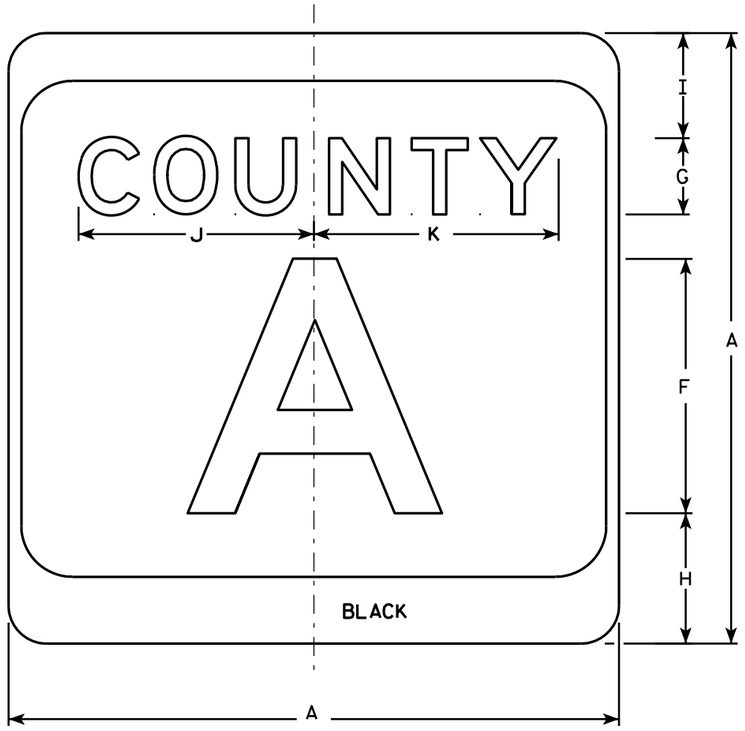
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

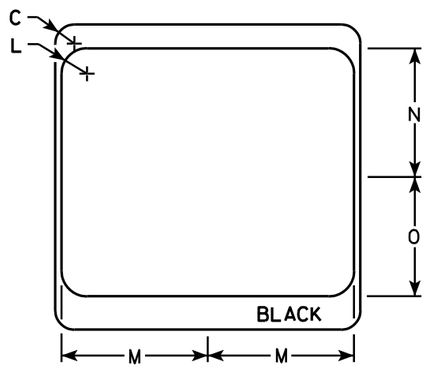
DATE 4/26/16 PLATE I55-56B.2

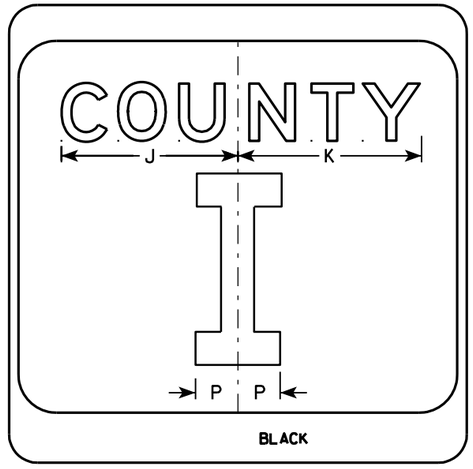
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

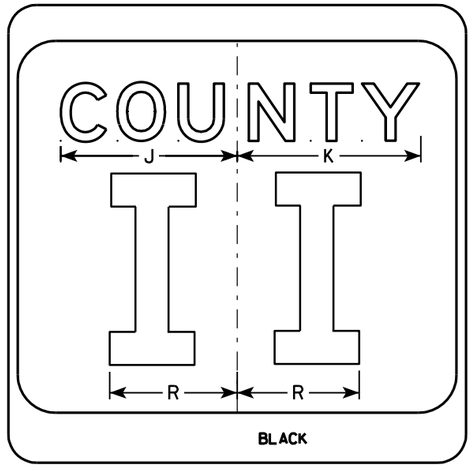
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\I5556B.DGN PLOT DATE : 26-APR-2016 17:29 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 1.945842:1.000000
WISDOT/CADDS SHEET 42



M1-5A







NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black - See Note 7
   Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
   Message Series D for 2 letters unless message is too big then Series C.
   Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
   Background - Type H Reflective
   Detour or temporary Signs
   Background - Reflective

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

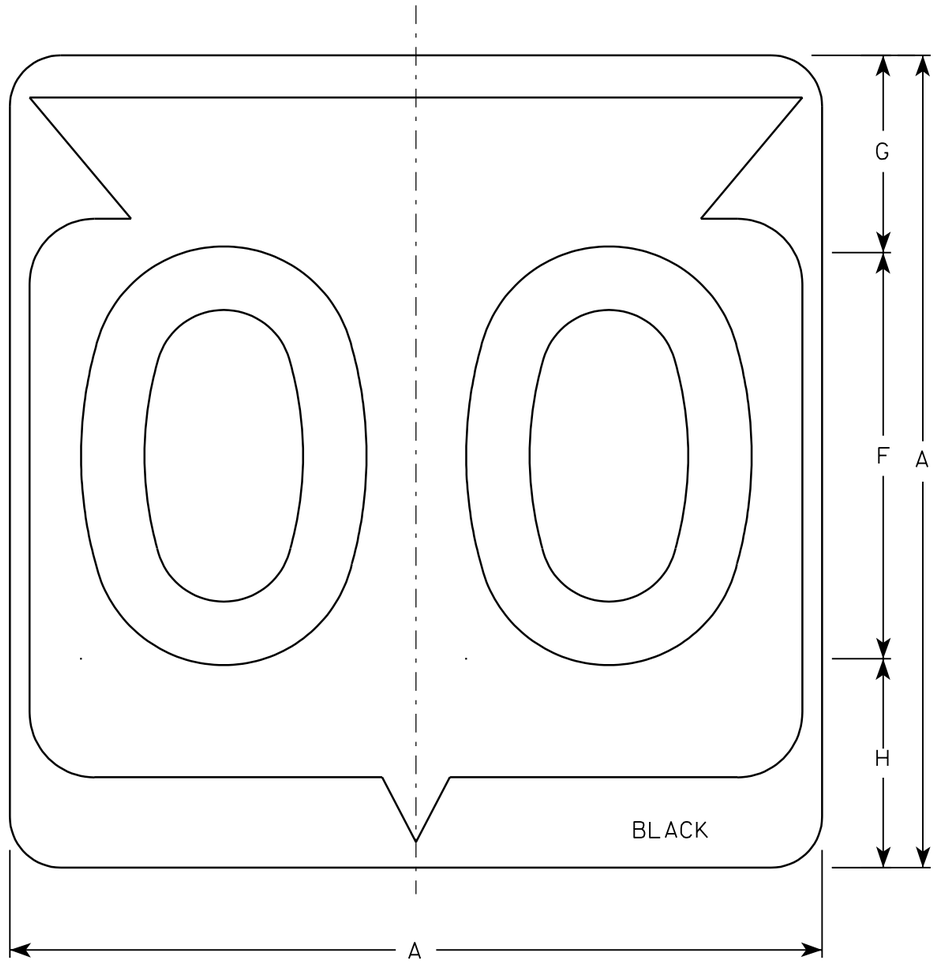
CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/27/11 PLATE NO. M1-5A.8

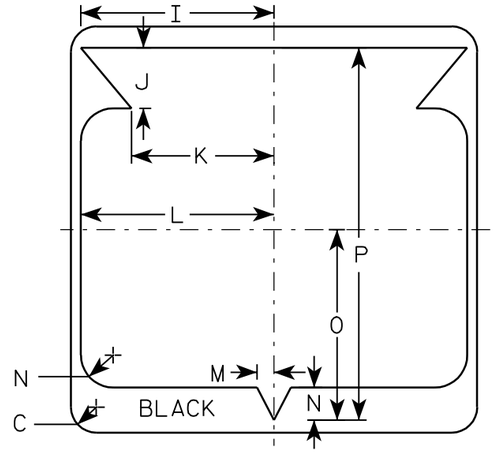
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 115	E



M1-6

NOTES

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black - See Note 6
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
6. Permanent Signs
   Background - Type H Reflective
   Detour or temporary Signs
   Background - Reflective



Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81

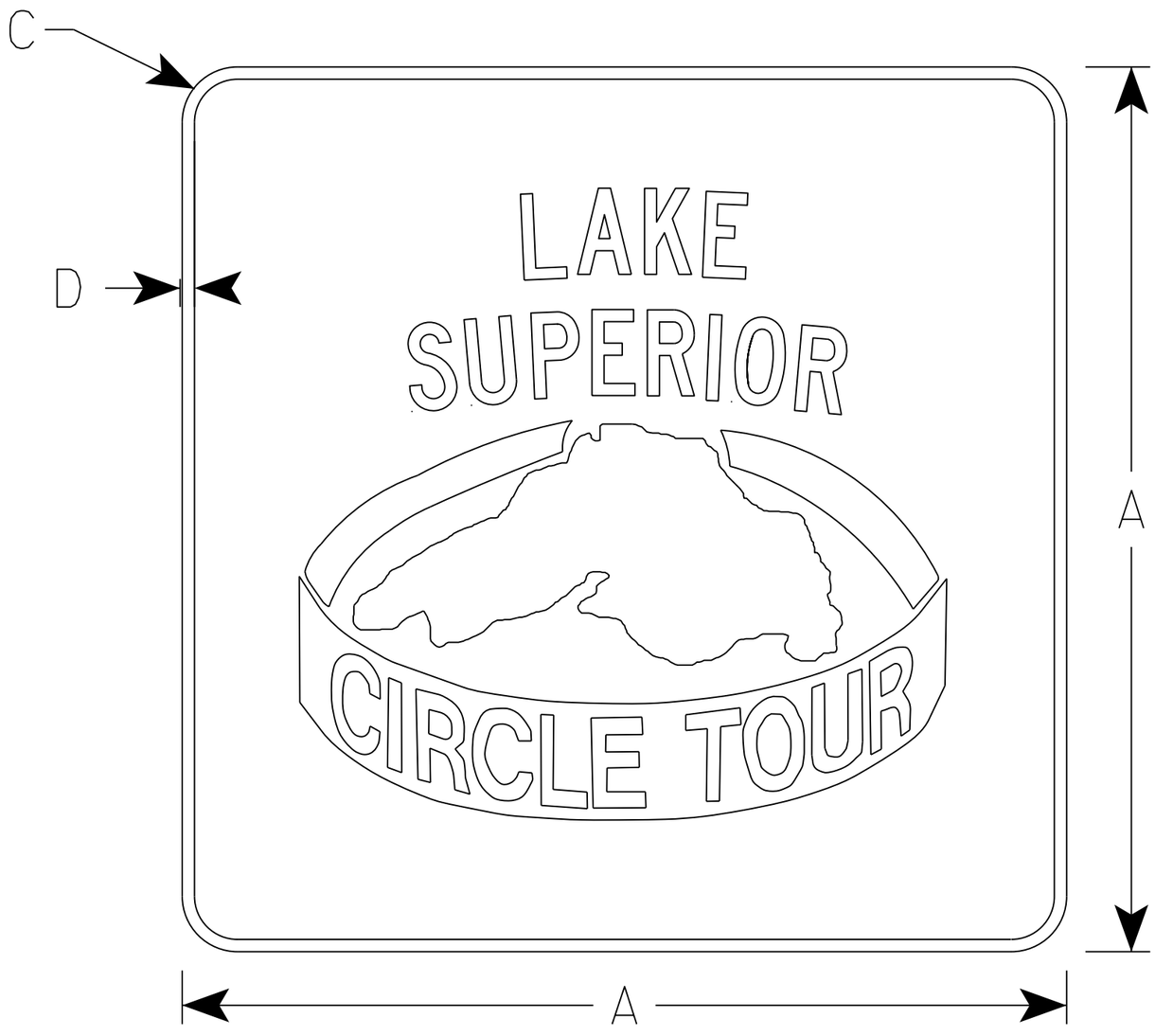
STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Chester J Spang for State Traffic Engineer

DATE 3/20/02 PLATE NO. M1-6.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 116	E



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Green
   Message - White - Graphics - White
   Circle Tour Message is Green
3. Message Series - Special
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	24		1 1/8	1/2																							4.0	.36
3																												
4	36		1 5/8	3/4																							9.0	.81
5																												

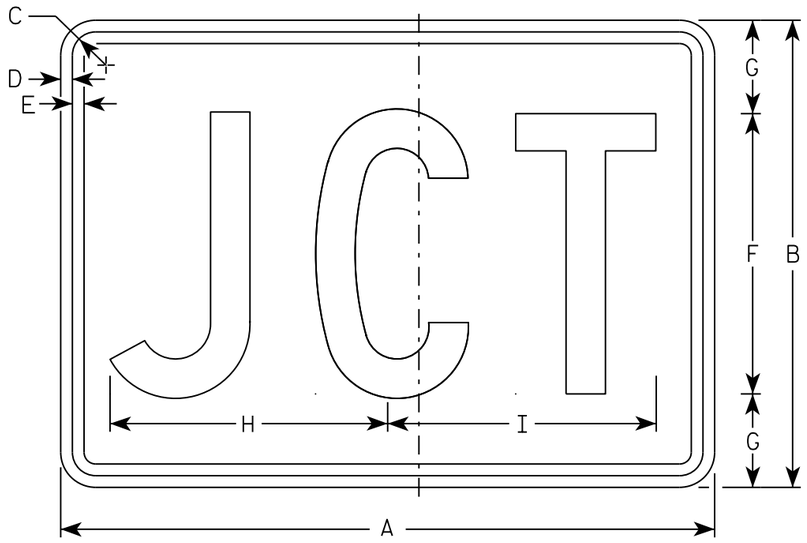
STANDARD SIGN
M1-91

WISCONSIN DEPT OF TRANSPORTATION

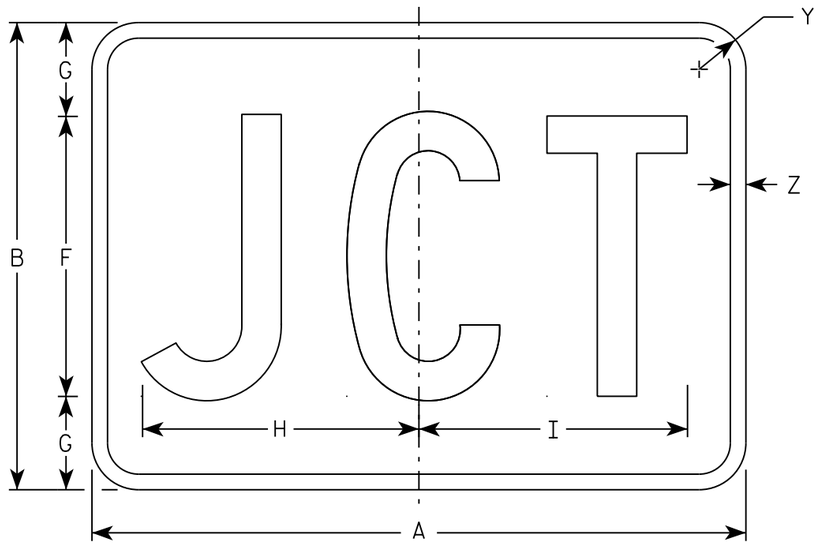
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/5/15 PLATE NO. M1-91.2

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E



M2-1
MM2-1
MP2-1



MB2-1
MK2-1
MN2-1
MR2-1

NOTES

1. Sign is Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White
   Message - Black
   MB2-1 Background - Blue
   Message - White
   MK2-1 Background - Green
   Message - White
   MM2-1 Background - White
   Message - Green
   MN2-1 Background - Brown
   Message - White
   MP2-1 Background - White
   Message - Blue
   MR2-1 Background - Brown
   Message - Yellow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

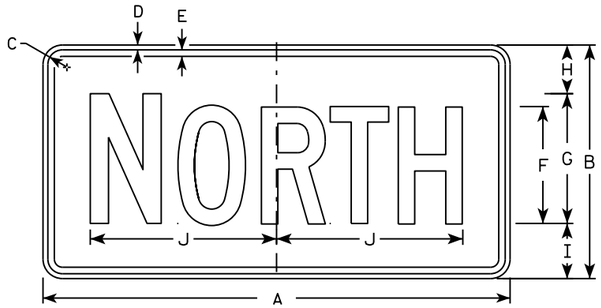
STANDARD SIGN
M2-1

WISCONSIN DEPT OF TRANSPORTATION

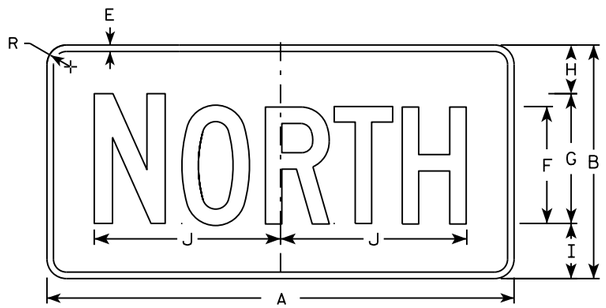
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 118 12-1.12

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



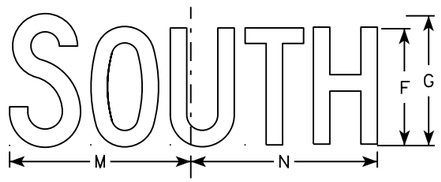
M3-1
MM3-1
MP3-1



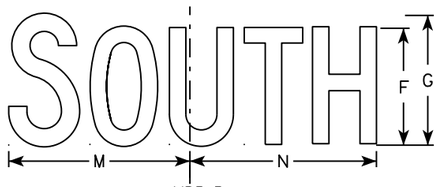
MB3-1
MK3-1
MN3-1

M3-2
MM3-2
MP3-2

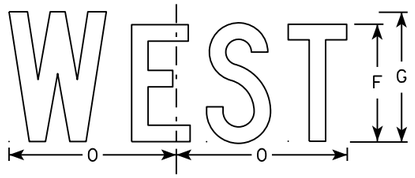
MB3-2
MK3-2
MN3-2



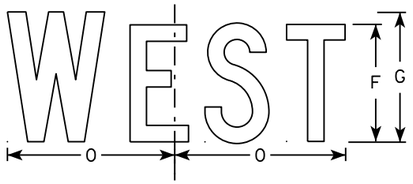
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
   Message - Black
   MB3-1 thru MB3-4 Background - Blue
   Message - White
   MK3-1 thru MK3-4 Background - Green
   Message - White
   MM3-1 thru MM3-4 Background - White
   Message - Green
   MN3-1 thru MN3-4 Background - Brown
   Message - White
   MP3-1 thru MP3-4 Background - White
   Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thur M3-4
SERIES

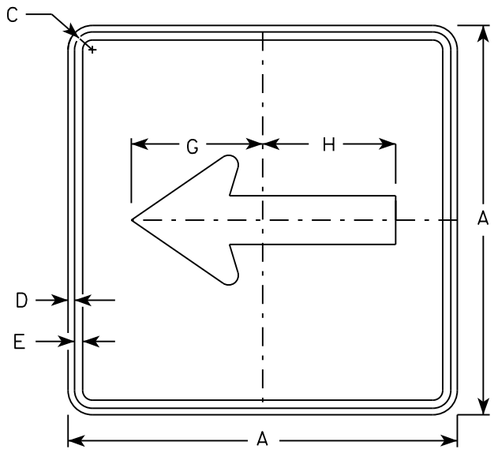
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew P Rauch for State Traffic Engineer

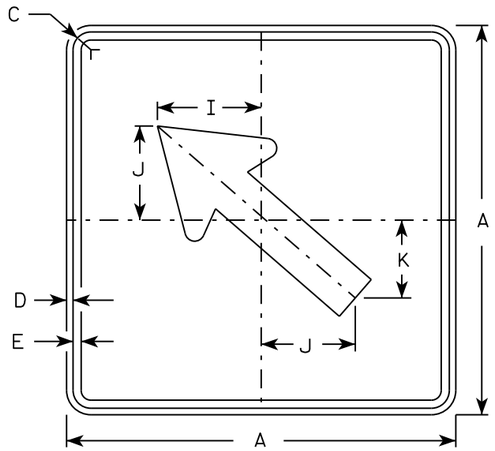
DATE 10/15/15 PLATE NO M3-1.14

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M31.DGN PLOT DATE : 01-DEC-2015 17:54 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 11.675051:1.000000



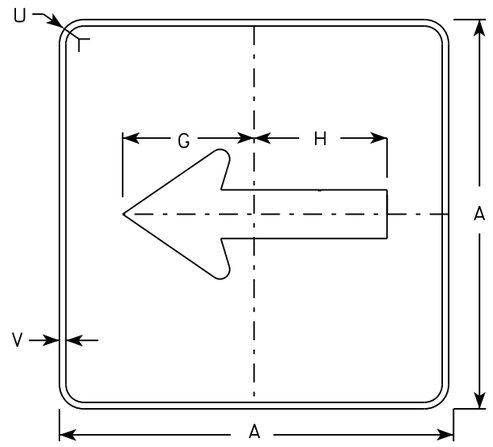
M6-1
MM6-1
MO6-1
MP6-1



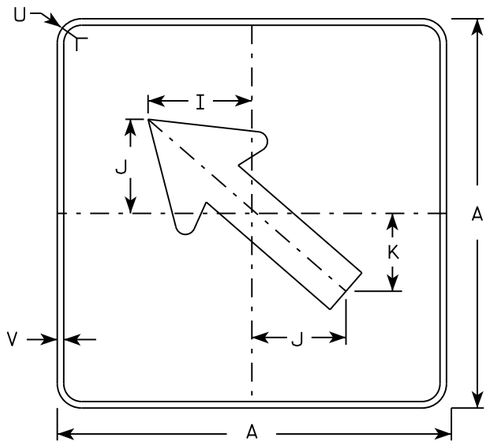
M6-2
MM6-2
MO6-2
MP6-2

NOTES

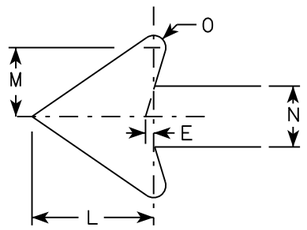
1. Signs are Type II - Type H except as Shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-1 and M6-2 Background - White
   Message - Black
   MB6-1 and MB6-2 Background - Blue
   Message - White
   MK6-1 and MK6-2 Background - Green
   Message - White
   MM6-1 and MM6-2 Background - White
   Message - Green
   MN6-1 and MN6-2 Background - Brown
   Message - White
   MO6-1 and MO6-2 Background - Orange - Type F Reflective
   Message - Black
   MP6-1 and MP6-2 Background - White
   Message - Blue
   MR6-1 and MR6-2 Background - Brown
   Message - Yellow



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2						3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25

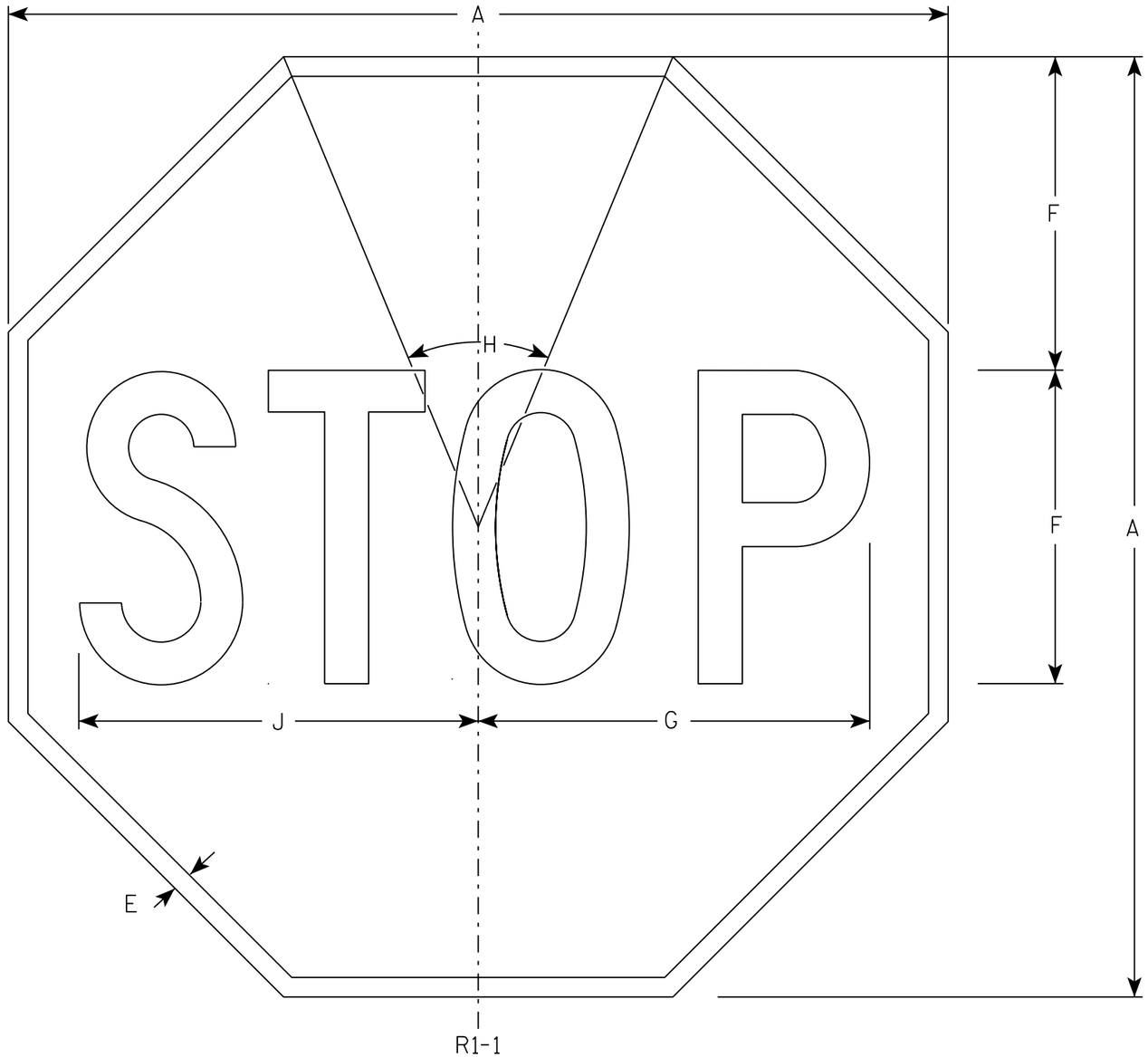
STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 120 M6-1.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



R1-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN
R1-1

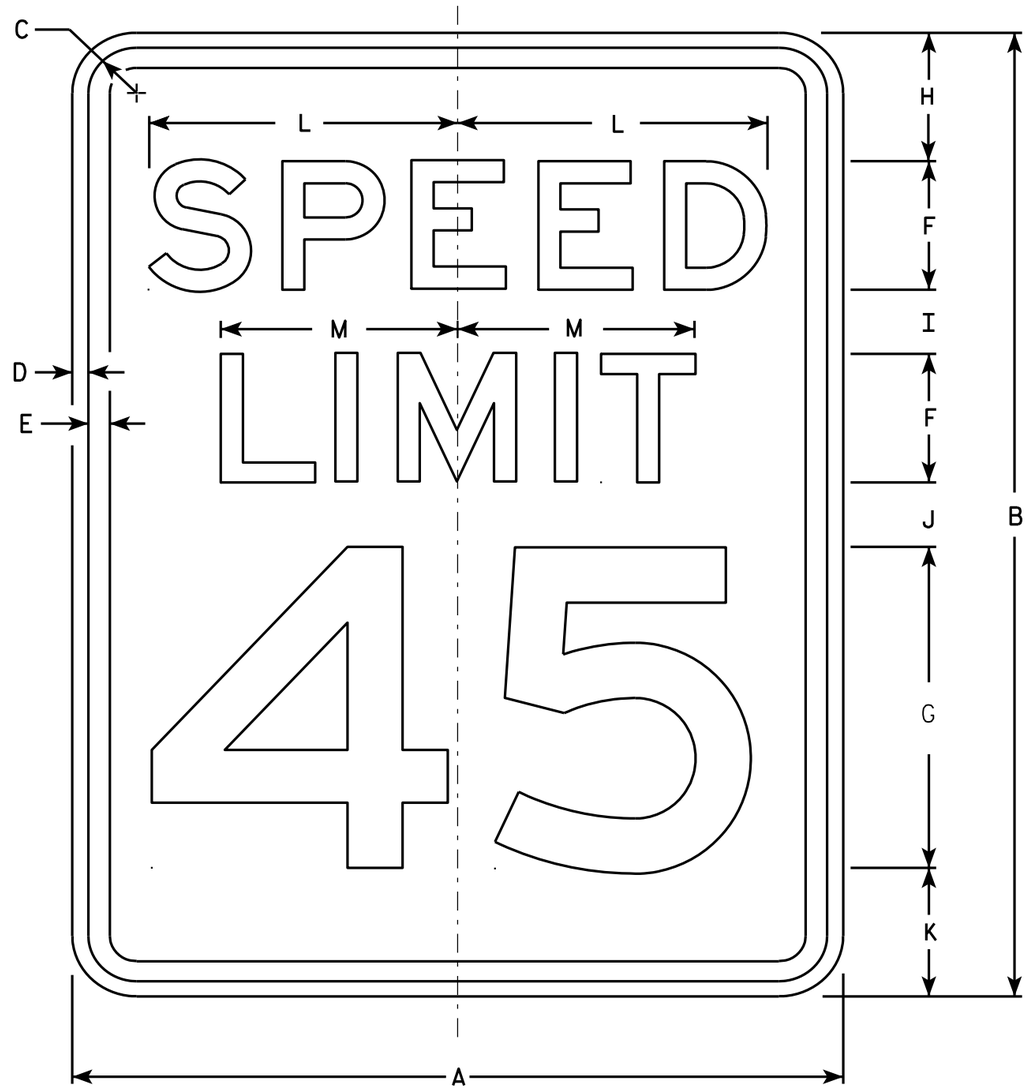
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/12/15 PLATE 121 R1-1.13

PROJECT NO:	HWY:	COUNTY:		SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\R11.DGN PLOT DATE : 01-DEC-2015 18:07 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 4.378143:1.000000



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

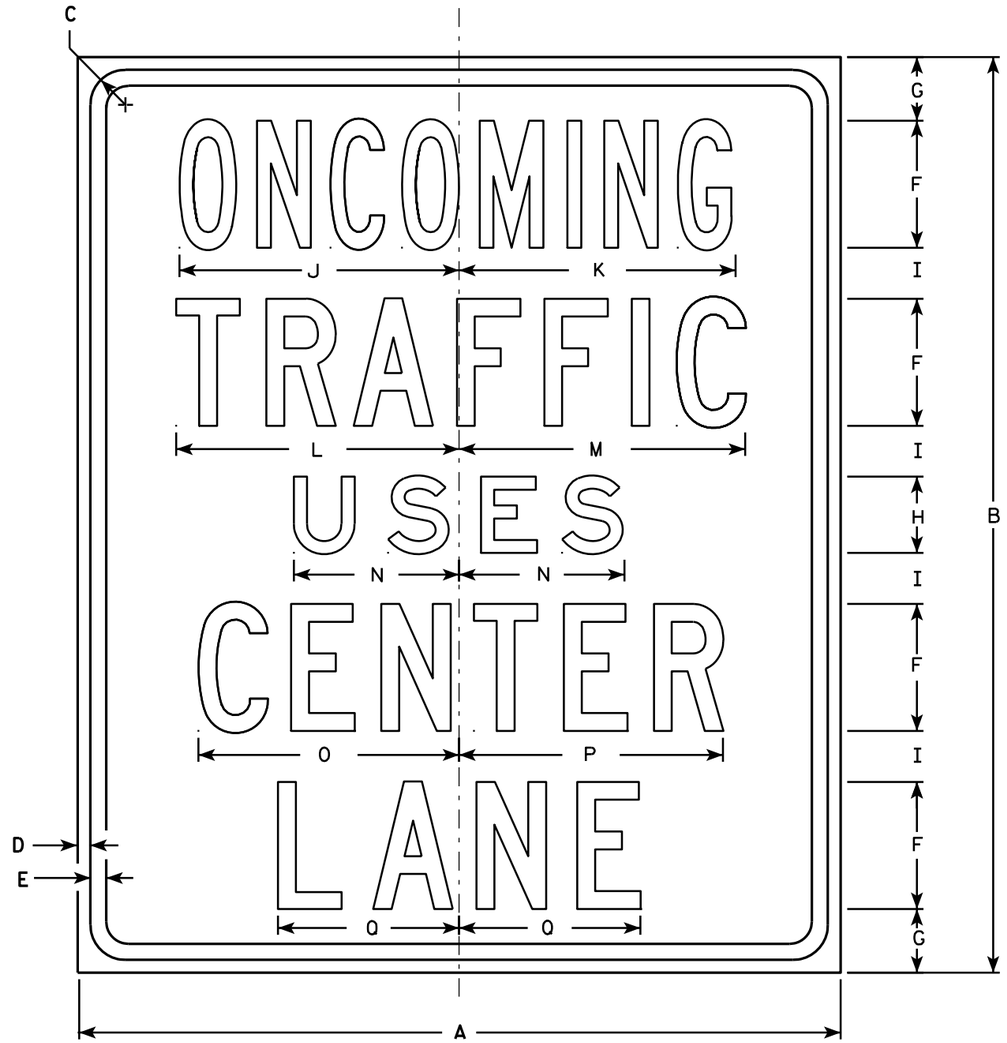
STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 122 | E



R3-72

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - Line 1 is Series B, Lines 2, 4 and 5 are Series C, and Line 3 is Series E.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	36	1 3/8	1/2	5/8	5	2 1/2	3	2	11	10 7/8	11 1/8	11 1/4	6 1/2	10 1/4	10 3/8	7 1/8										7.50
2S	30	36	1 3/8	1/2	5/8	5	2 1/2	3	2	11	10 7/8	11 1/8	11 1/4	6 1/2	10 1/4	10 3/8	7 1/8										7.50
2M	30	36	1 3/8	1/2	5/8	5	2 1/2	3	2	11	10 7/8	11 1/8	11 1/4	6 1/2	10 1/4	10 3/8	7 1/8										7.50
3																											
4																											
5																											

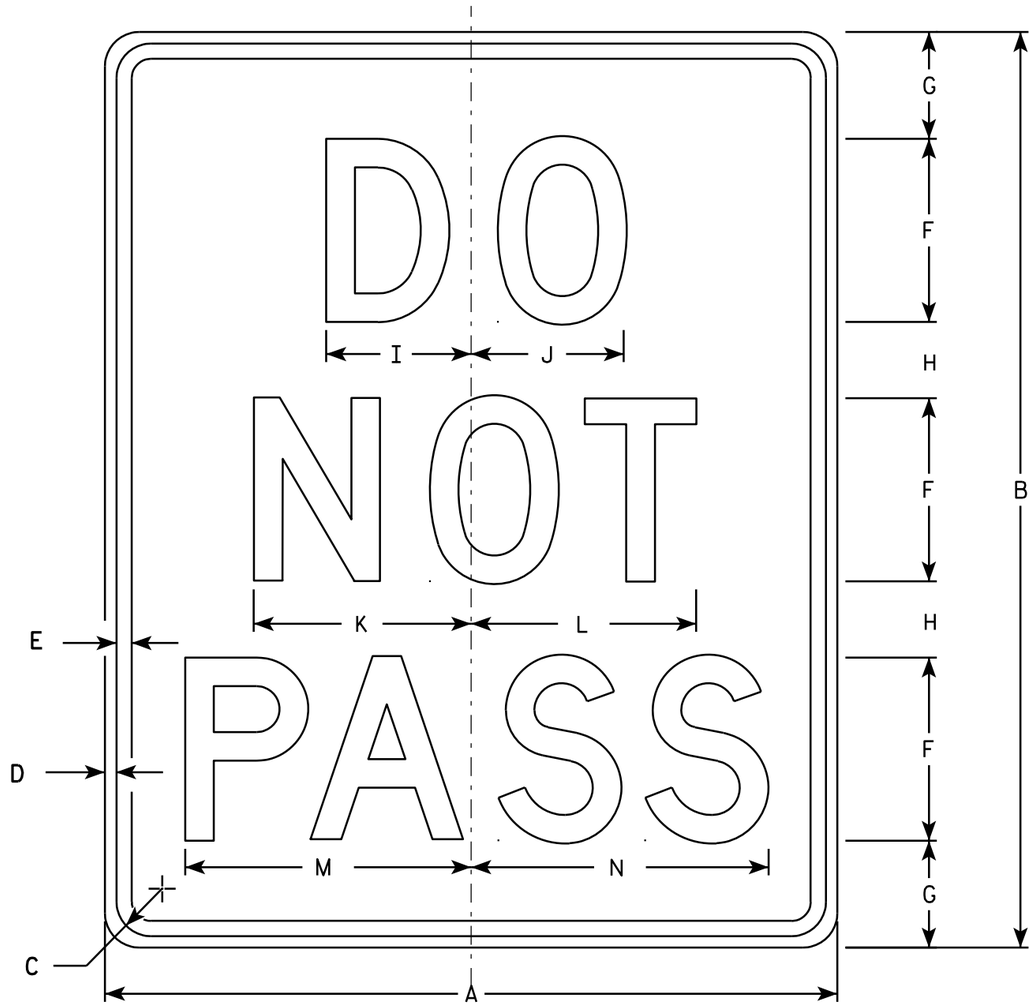
STANDARD SIGN
R3-72

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R3-72.5

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 123	E



R4-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	4	3 1/2	2 1/2	3 1/8	3 1/4	4 3/4	4 7/8	6 1/4	6 1/2													3.0
2S	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
2M	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
3																											
4	36	48	1 5/8	5/8	3/4	8	7	5	6 1/4	6 5/8	9 1/2	9 3/4	12 1/2	13													12.0
5	48	60	2 1/4	3/4	1	10	8	7	7 3/4	8 3/8	11 7/8	12 1/4	15 5/8	16 1/4													20.0

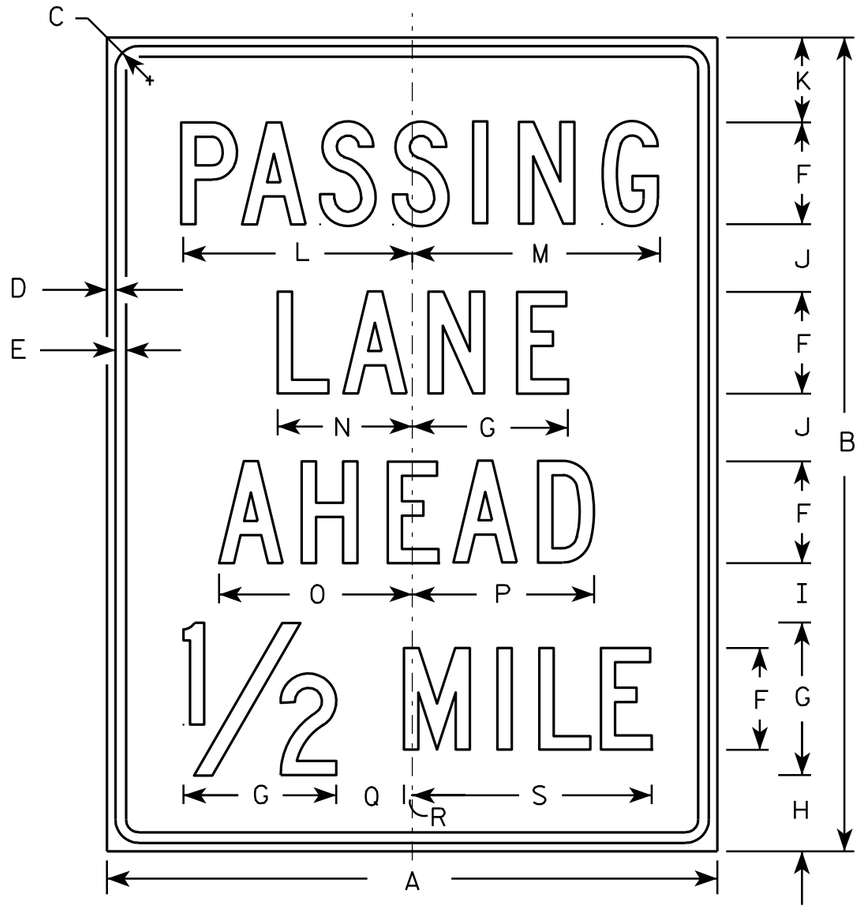
STANDARD SIGN
R4-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-1.7

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 124 | E



R4-51

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	48	1 3/8	1/2	5/8	6	9	4 1/2	3 1/2	4	5	13 5/8	14 1/2	8	11 3/8	10 3/4	4	1/2	14 1/8								12.0
2M	36	48	1 3/8	1/2	5/8	6	9	4 1/2	3 1/2	4	5	13 5/8	14 1/2	8	11 3/8	10 3/4	4	1/2	14 1/8								12.0
3																											
4																											
5																											

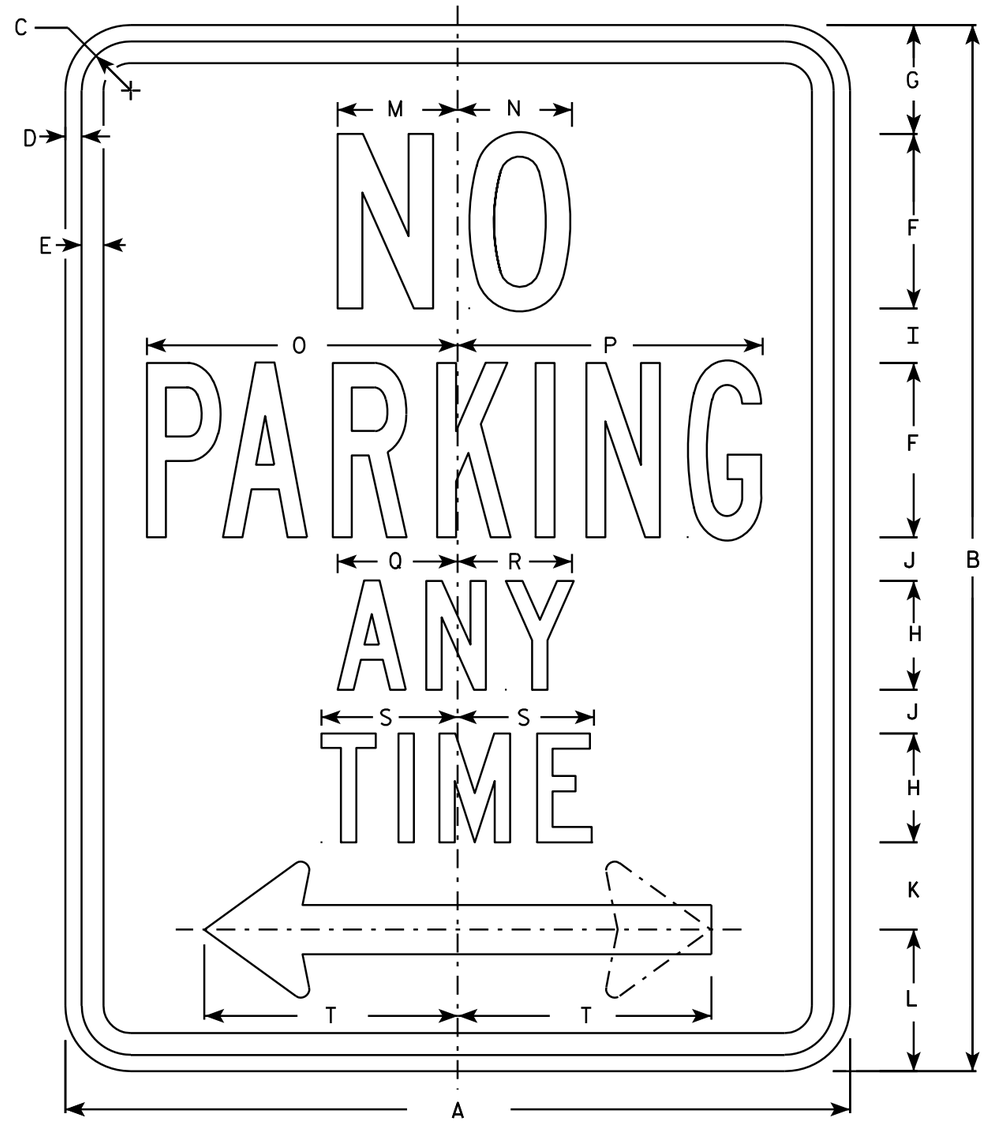
STANDARD SIGN
R4-51

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/29/2011 PLATE NO. R4-51.4

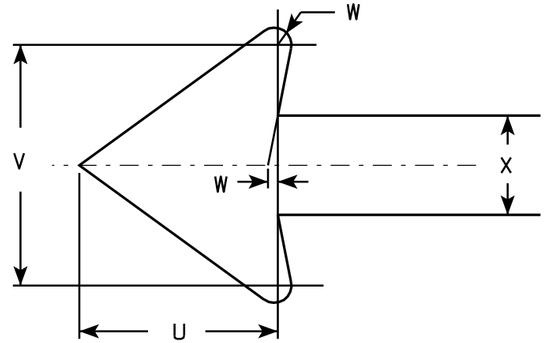
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 125 | E



R7-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)
   R7-1L (left arrow)
   R7-1R (right arrow)



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

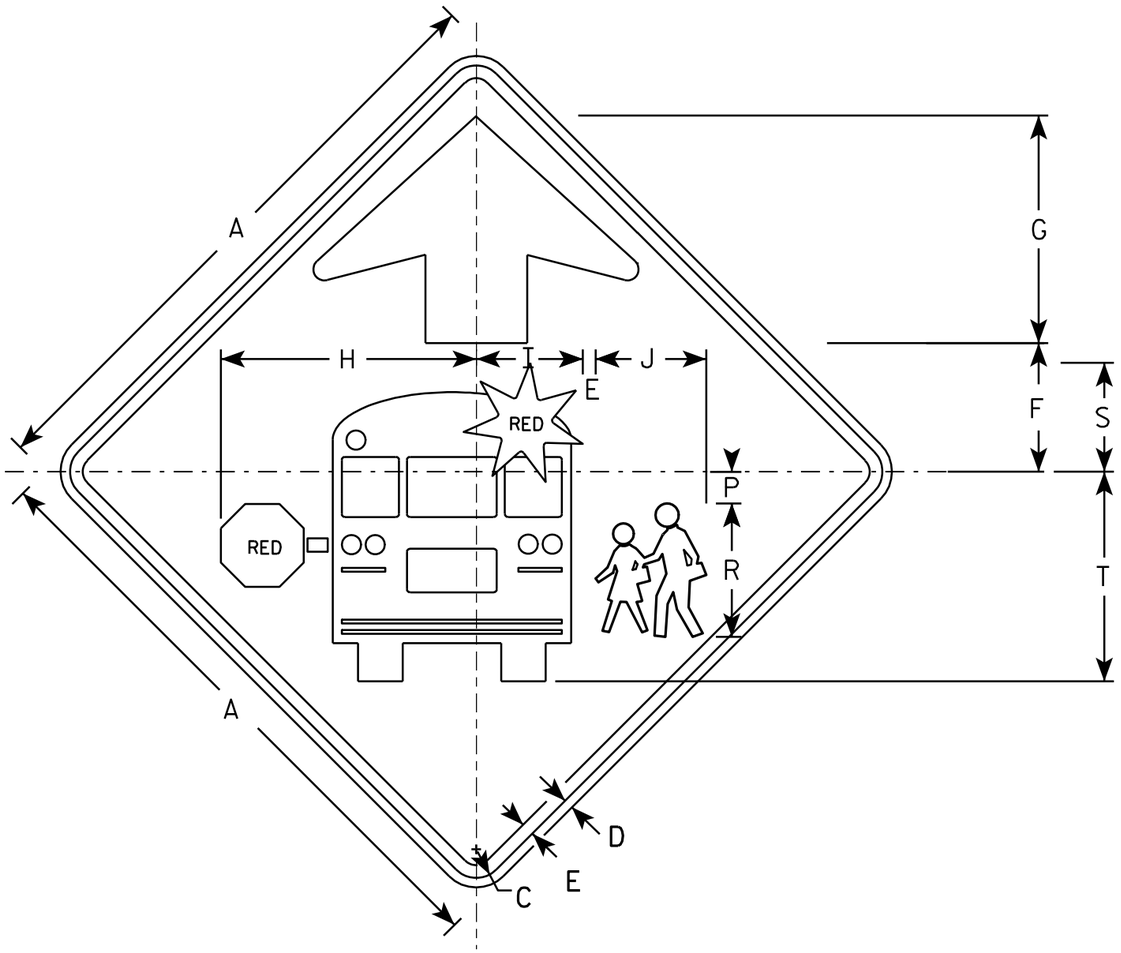
STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

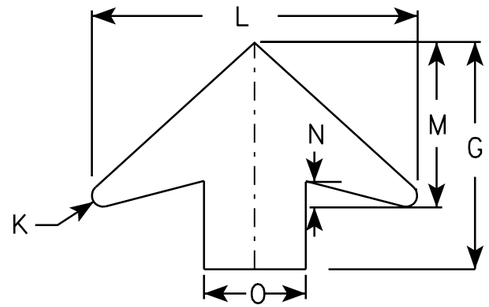
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 126 | E



S3-1

NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - YELLOW-GREEN
   Message - BLACK except as noted
   Circles except PEDS- RED BACKGROUND
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	12 1/2	5 1/4	5 1/2	1/2	16	8	1 1/4	5	1 1/2		6 5/8	5 3/8	10 3/8							6.25
2	36		1 5/8	5/8	3/4	7 1/2	13 1/2	15 1/8	6 1/4	6 1/2	5/8	19 1/4	9 3/4	1 5/8	6	1 7/8		7 7/8	6 3/8	12 3/8							9.0
3	48		2 1/4	3/4	1	10	17 7/8	20 1/8	8 3/8	8 3/4	7/8	25 5/8	13	2	8	2 1/2		10 1/2	8 1/2	16 1/2							16.0
4	48		2 1/4	3/4	1	10	17 7/8	20 1/8	8 3/8	8 3/4	7/8	25 5/8	13	2	8	2 1/2		10 1/2	8 1/2	16 1/2							16.0
5																											

STANDARD SIGN
S3-1

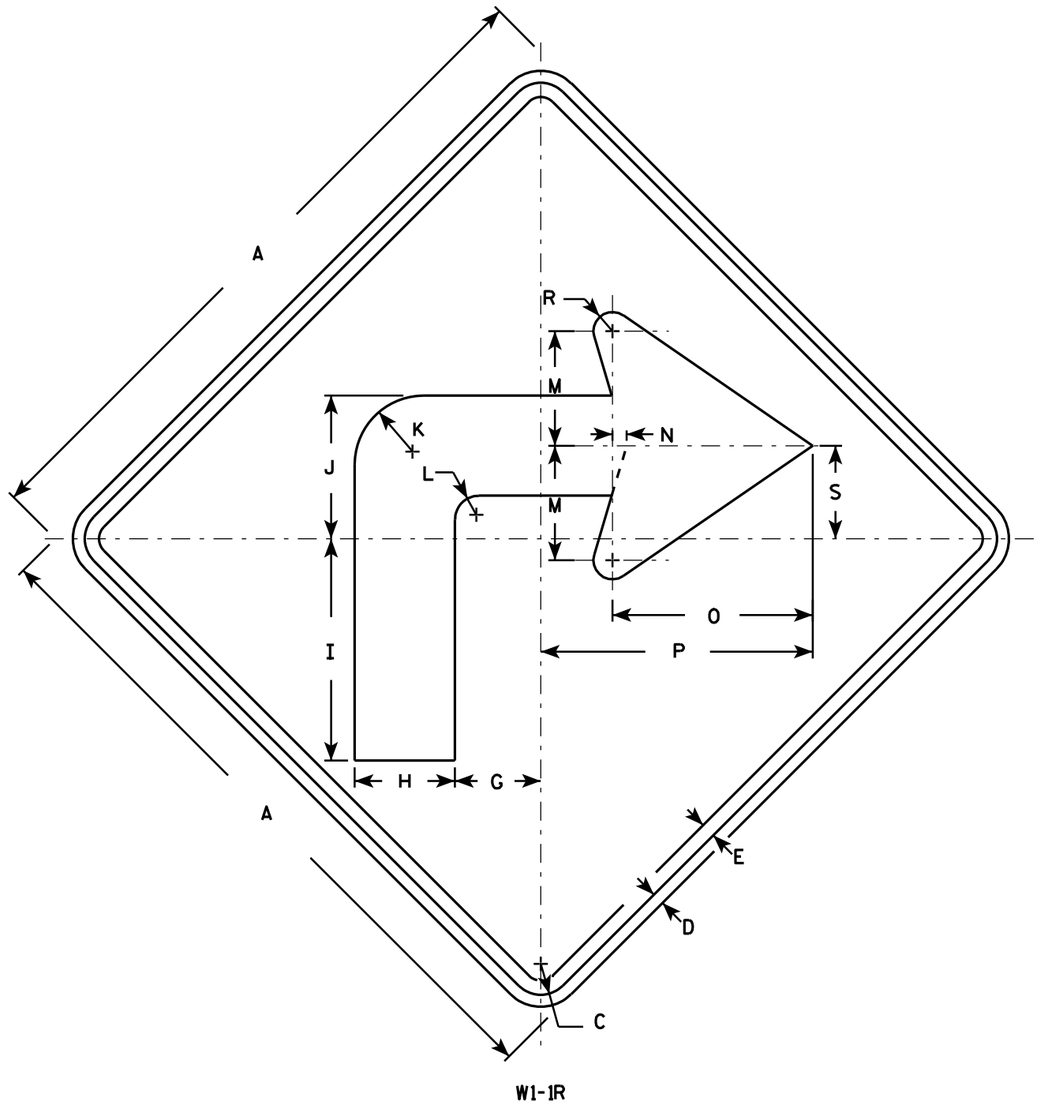
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/8/10 PLATE NO. S3-1.6

PROJECT NO:

SHEET NO: 127 E



W1-1R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-1L is the same as W1-1R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		3	3 1/2	7 3/4	5	2 1/2	7/8	4	1/2	7	9 1/2		5/8	3 1/4								4.0
2S	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
2M	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
3	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
4	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0
5	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0

STANDARD SIGN
W1-1

WISCONSIN DEPT OF TRANSPORTATION

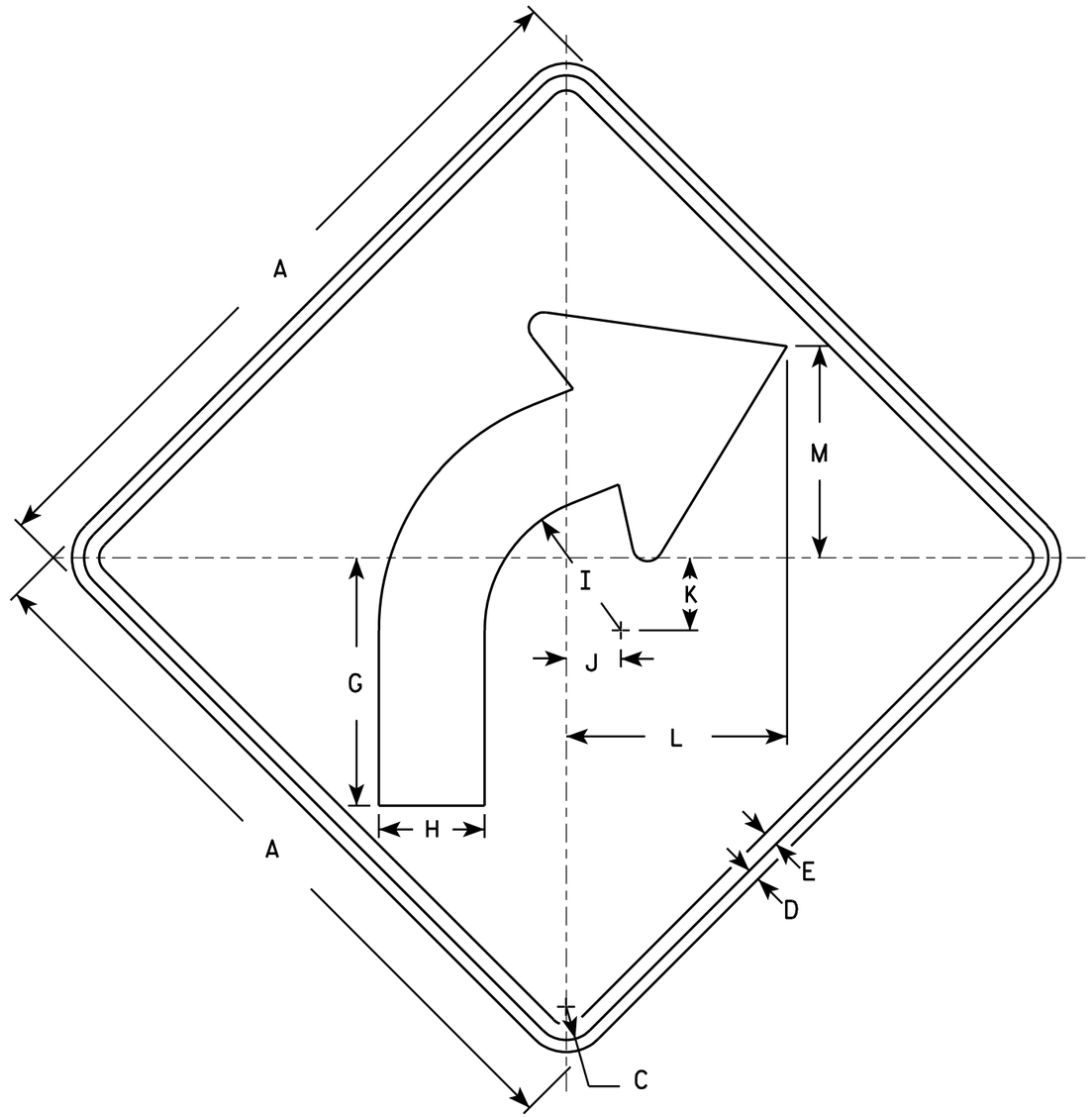
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-1.11

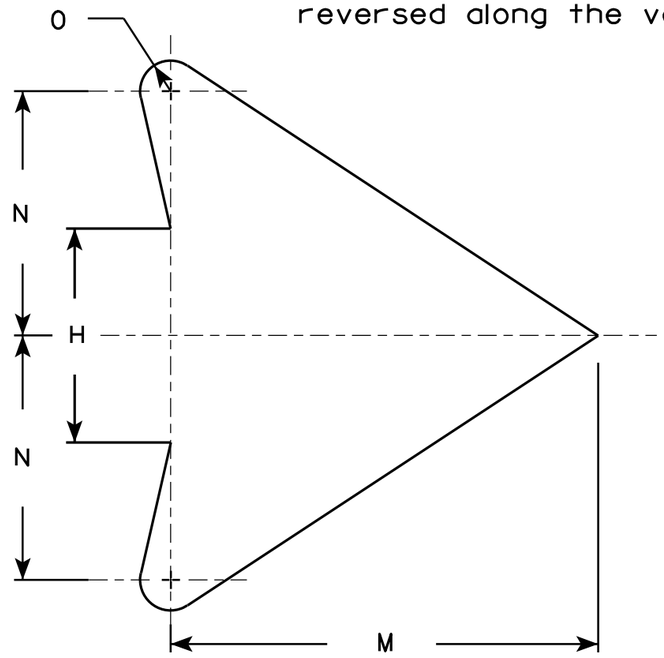
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 128 | E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

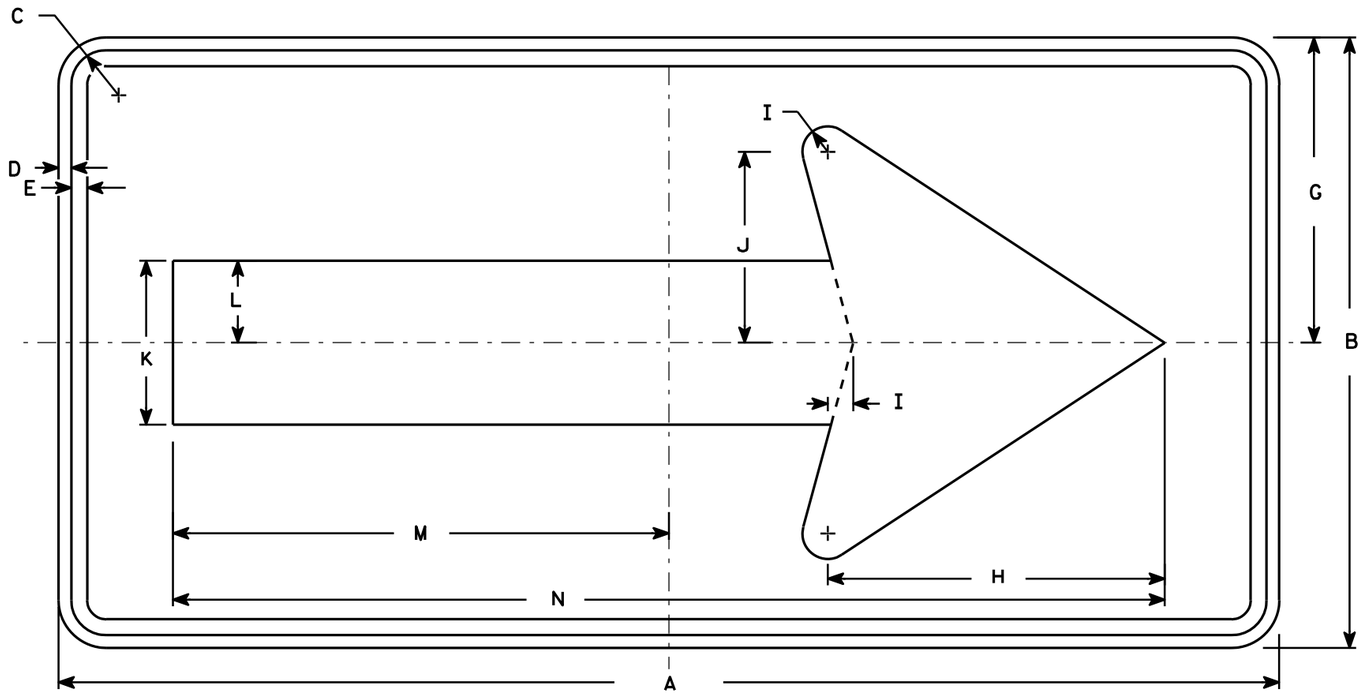
STANDARD SIGN
W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 129	E



W1-6

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	3/8		9	10	3/4	5 5/8	4 3/4	2 3/8	14 5/8	29 1/4													4.5
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	96	48	2 1/4	3/4	1		24	26 1/2	2	15	13	6 1/2	39	78													32.0

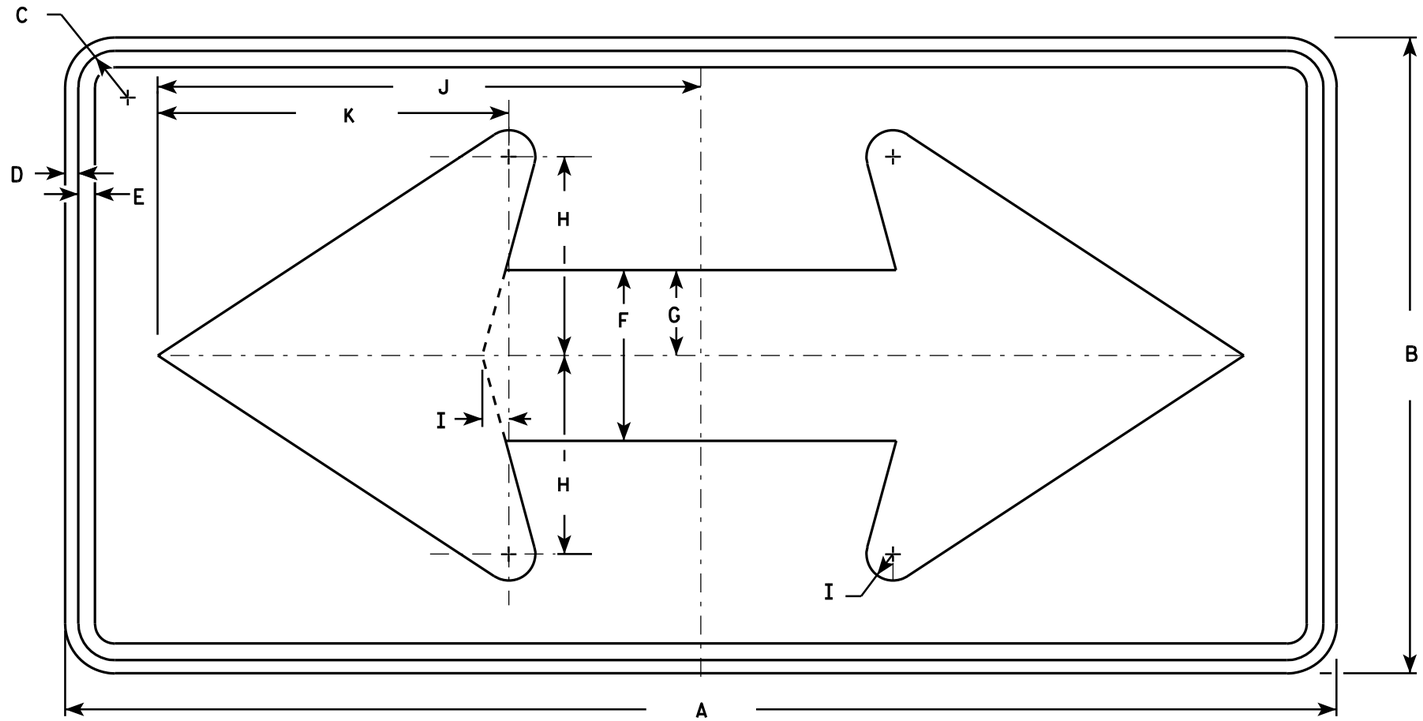
STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-6.8

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 130



W1-7

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 1/2	5 3/4	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

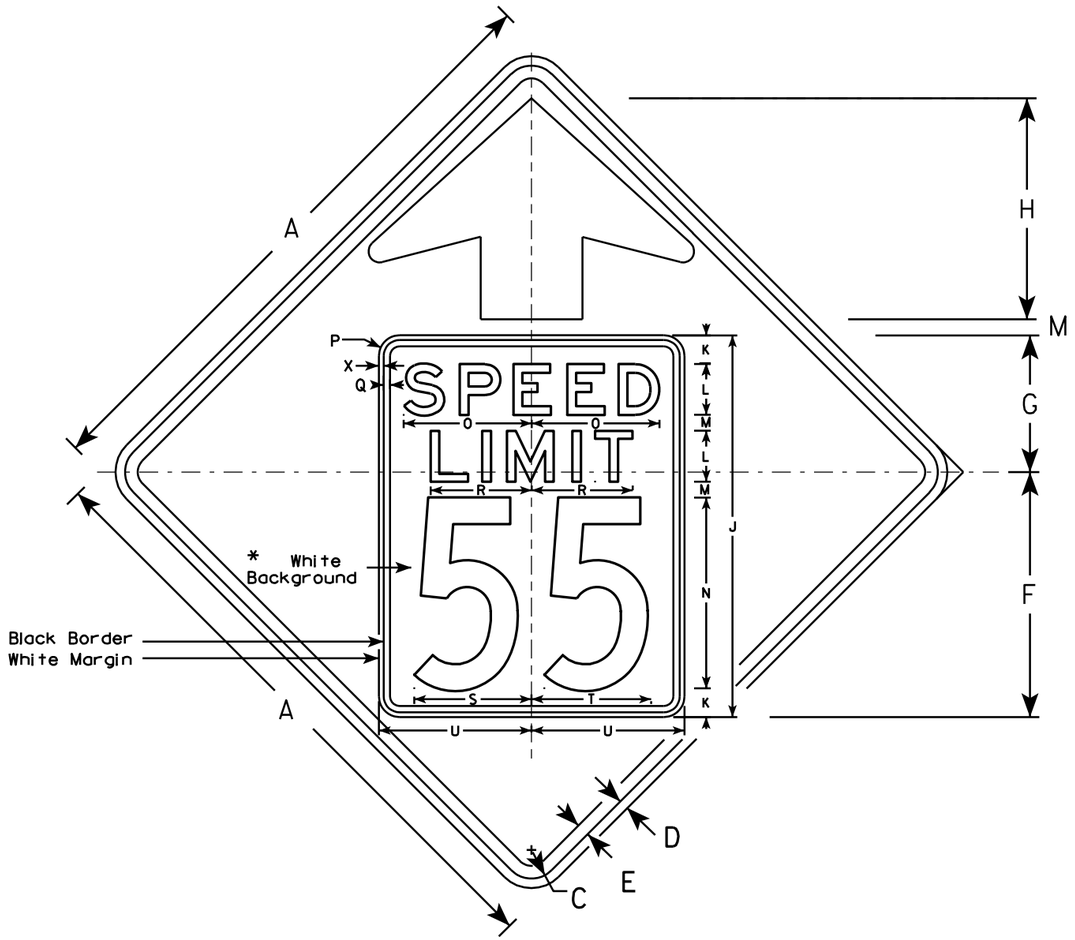
STANDARD SIGN
W1-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-7.7

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 131 | E

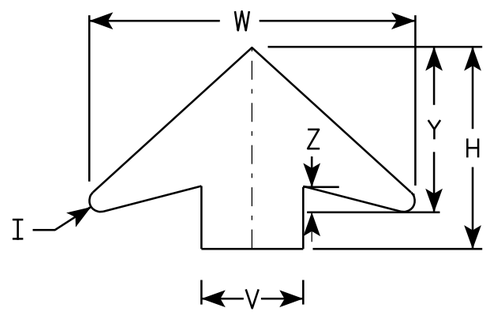


W3-5

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color: *
   Background - YELLOW*
   Message - BLACK
3. Message Series - C for numbers Series E for wording
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
2M	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
3	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
4	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0
5	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0

STANDARD SIGN
W3-5

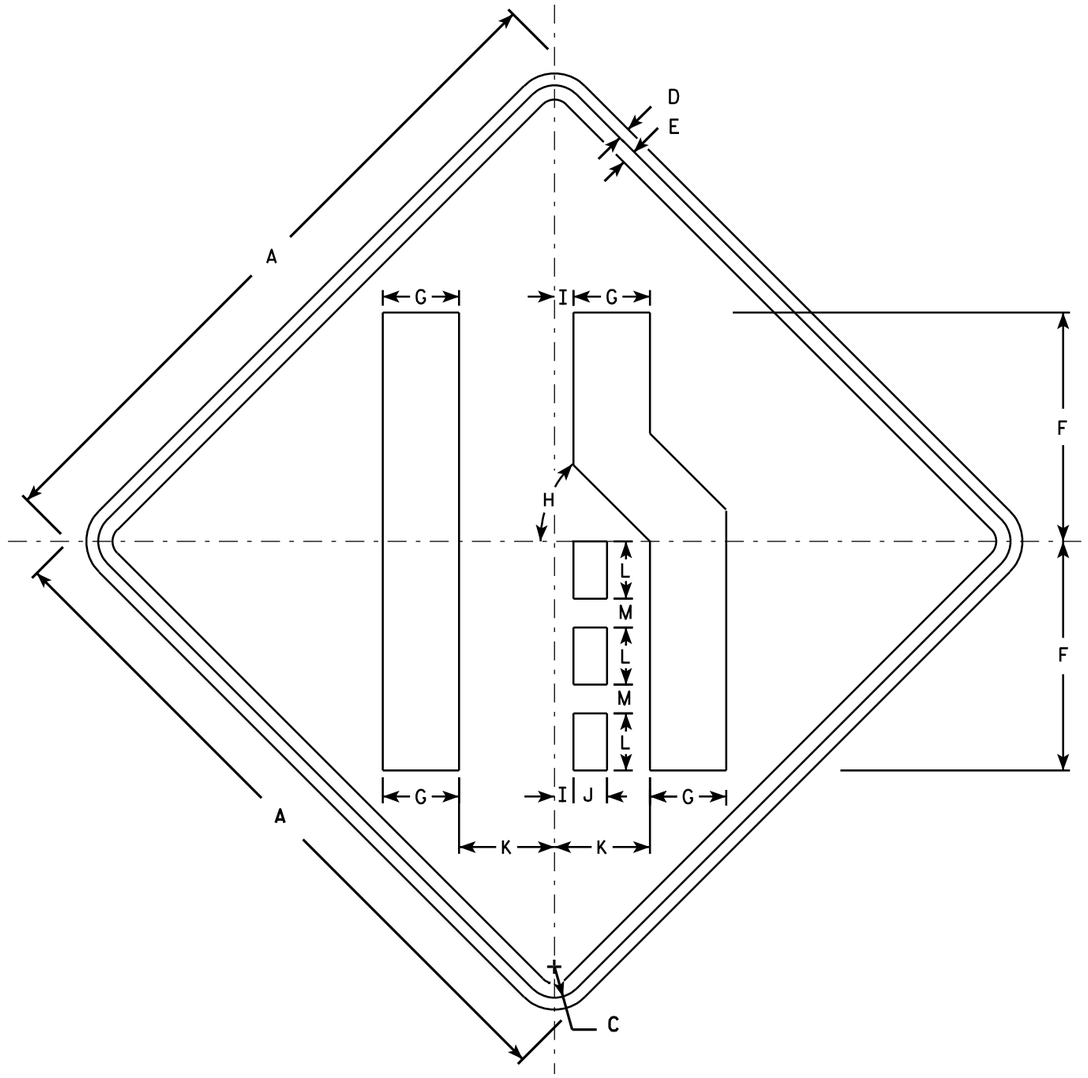
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W3-5.5

PROJECT NO:

SHEET NO: 132 E



W4-2R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W4-2L is the same as W4-2R except the symbol is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	10	3 3/8	45°	7/8	1 1/2	4 1/4	2 1/2	1 1/4														6.25
2S	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2M	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
3	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
4	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0

STANDARD SIGN
W4-2

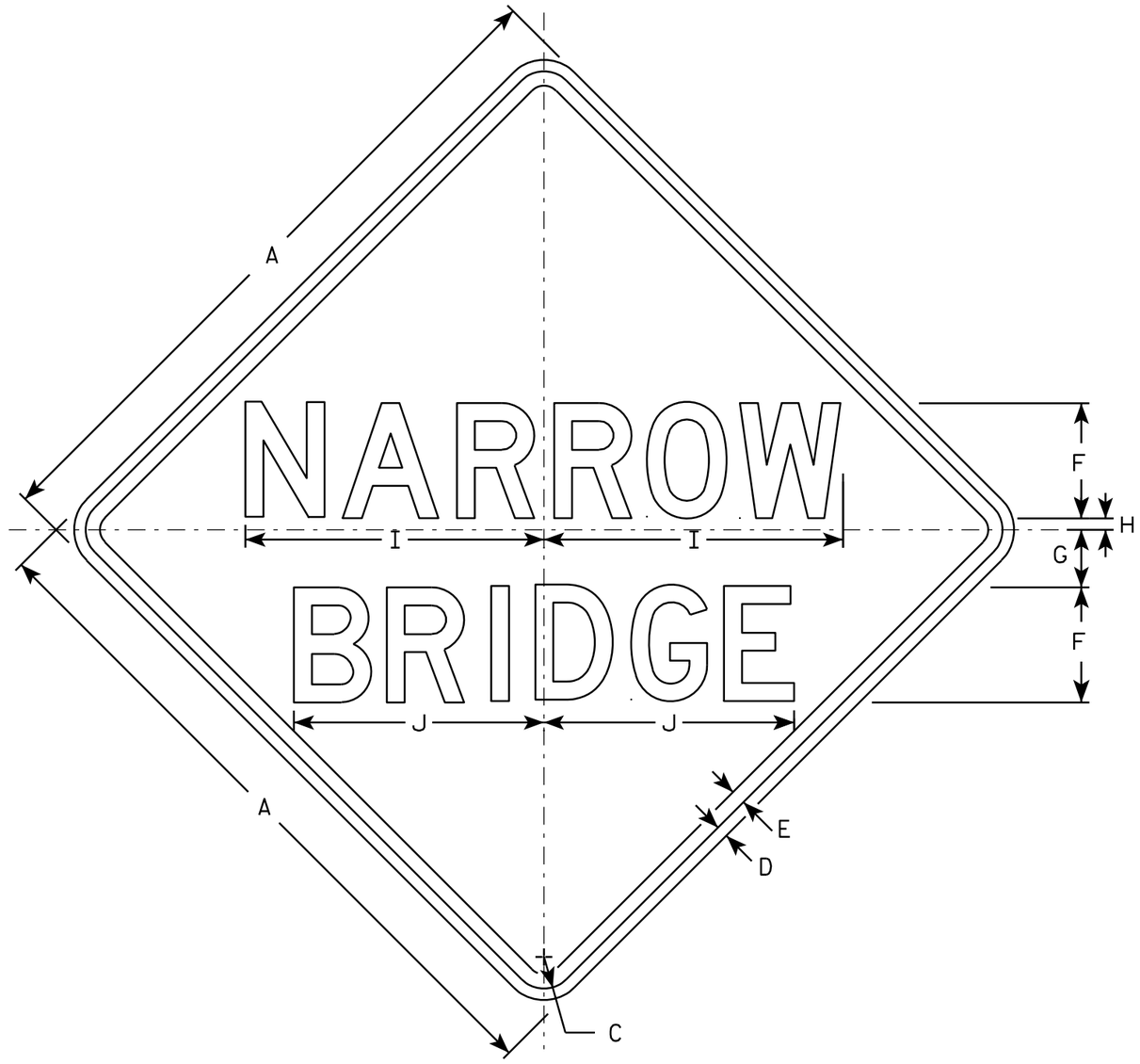
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/12/13 PLATE NO. W4-2.14

PROJECT NO:

SHEET NO: 133 E



W5-2

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	5	2 1/2	1/2	13	10 7/8																	6.25
2S	36		1 5/8	5/8	3/4	6	3	3/4	15 5/8	13 1/8																	9.0
2M	36		1 5/8	5/8	3/4	6	3	3/4	15 5/8	13 1/8																	9.0
3	36		1 5/8	5/8	3/4	6	3	3/4	15 5/8	13 1/8																	9.0
4	48		2 1/4	3/4	1	8	4	3/4	20 3/4	17 3/8																	16.0
5																											

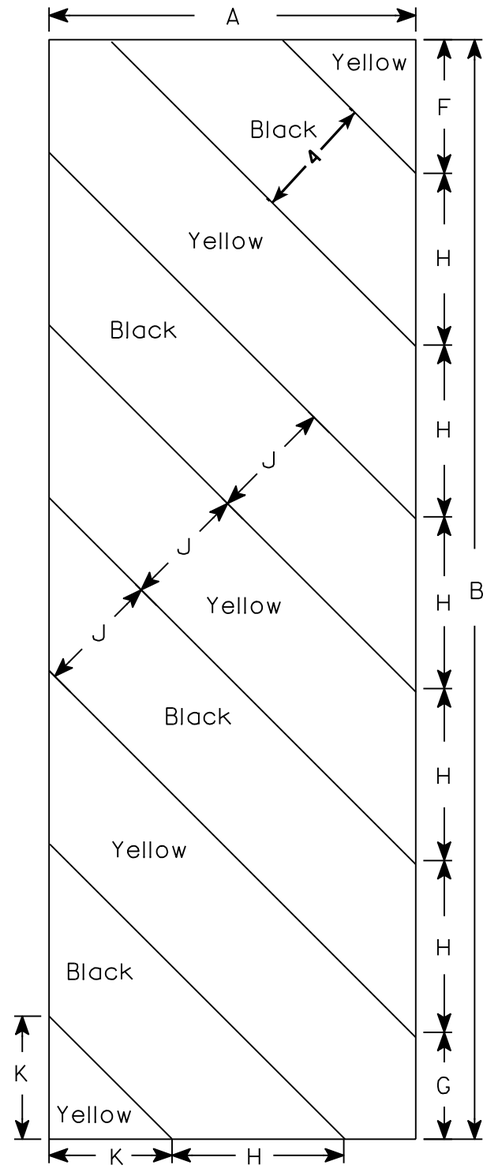
STANDARD SIGN
W5-2

WISCONSIN DEPT OF TRANSPORTATION

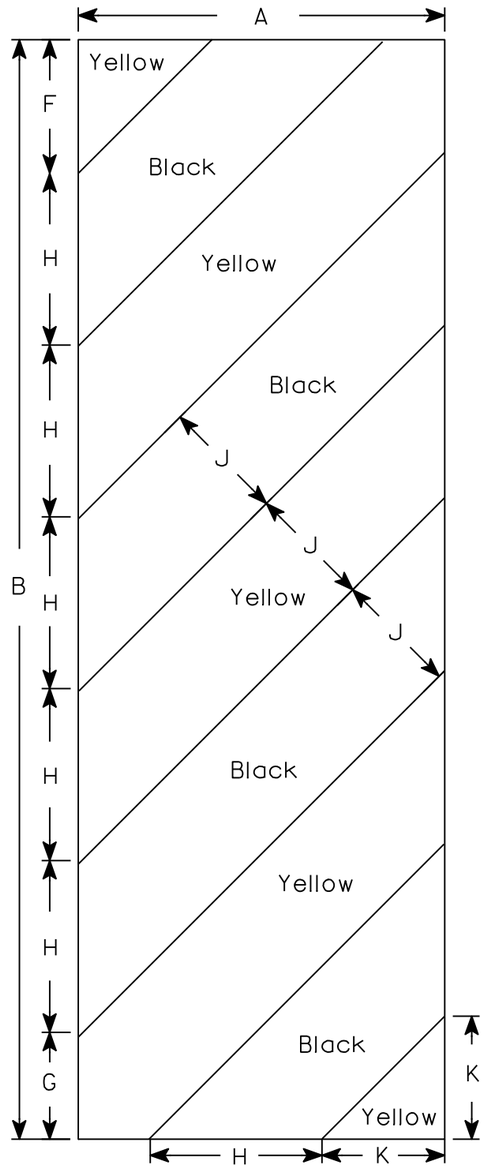
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 03/12/13 PLATE NO. W5-2.8

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 134	E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN

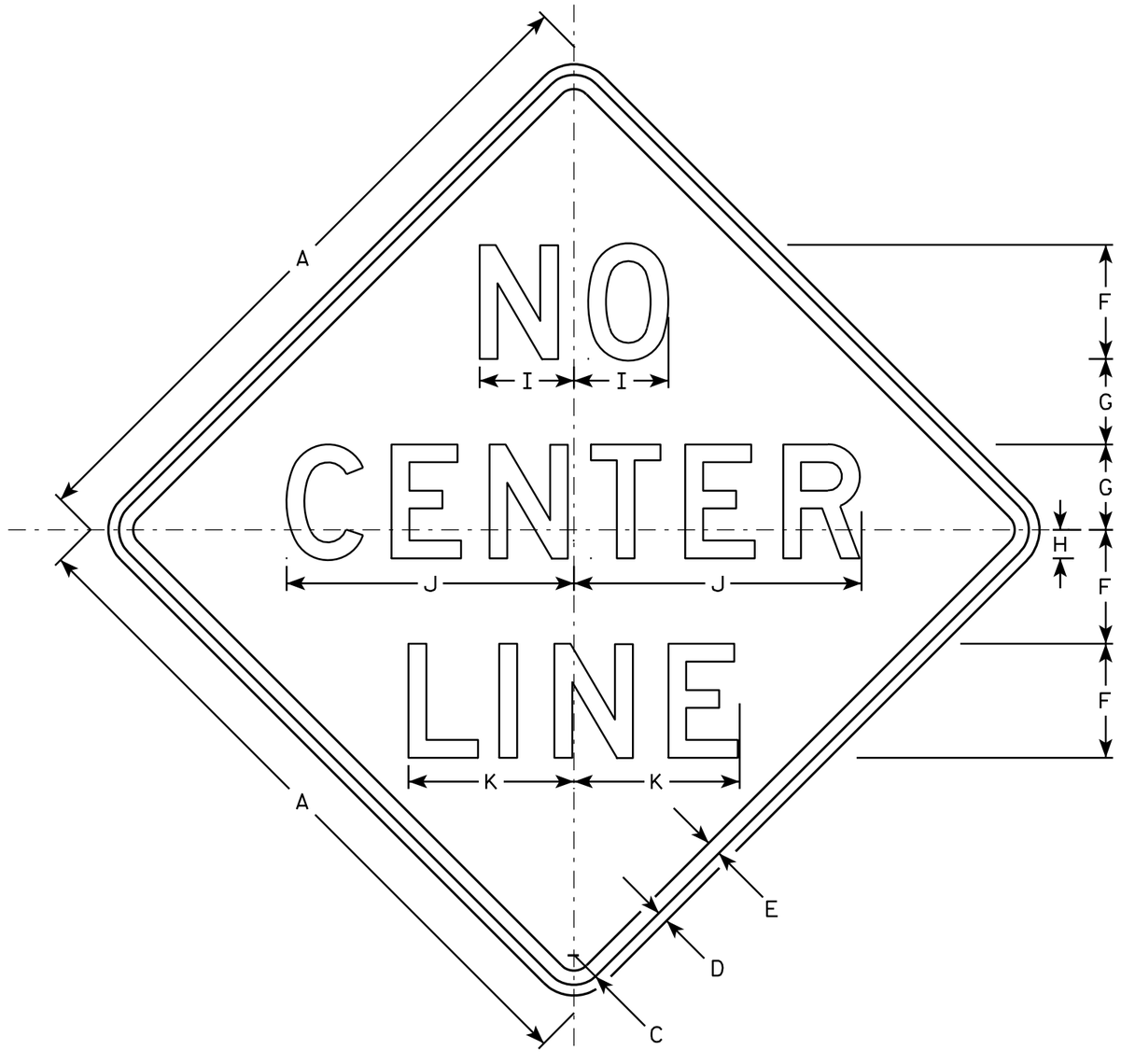
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 135	E



W8-12

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5/8	5/8	3/4	6	4 1/2	1 1/2	5 1/8	16	9																9.0
2M	36		1 5/8	5/8	3/4	6	4 1/2	1 1/2	5 1/8	16	9																9.0
3	48		2 1/4	3/4	1	8	6	2	6 5/8	20 1/4	11 5/8																16.0
4	48		2 1/4	3/4	1	8	6	2	6 5/8	20 1/4	11 5/8																16.0
5	48		2 1/4	3/4	1	8	6	2	6 5/8	20 1/4	11 5/8																16.0

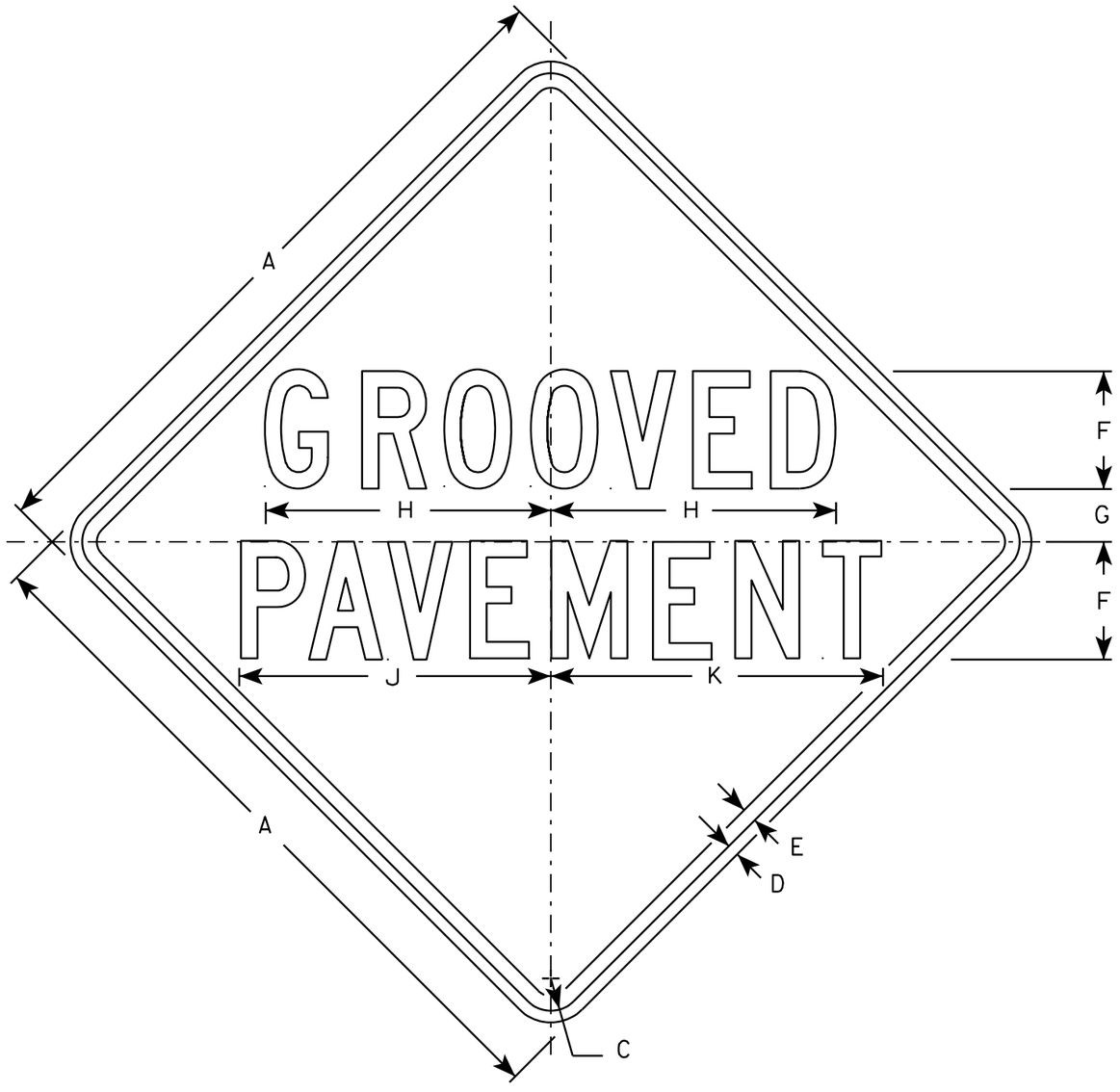
STANDARD SIGN
W8-12

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rausch
for State Traffic Engineer

DATE 8/24/10 PLATE NO. W8-12.3

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 136	E



W8-52

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 3/8	1/2	5/8	5	2 1/4	12 1/8		13 1/4	14 1/8																6.25
2M	36		1 5/8	5/8	3/4	6	2 5/8	14 1/2		15 7/8	17																9.0
3	36		1 5/8	5/8	3/4	6	2 5/8	14 1/2		15 7/8	17																9.0
4	36		1 5/8	5/8	3/4	6	2 5/8	14 1/2		15 7/8	17																9.0
5	48		2 1/4	3/4	1	8	3 1/2	19 3/8		21 1/4	22 5/8																16.0

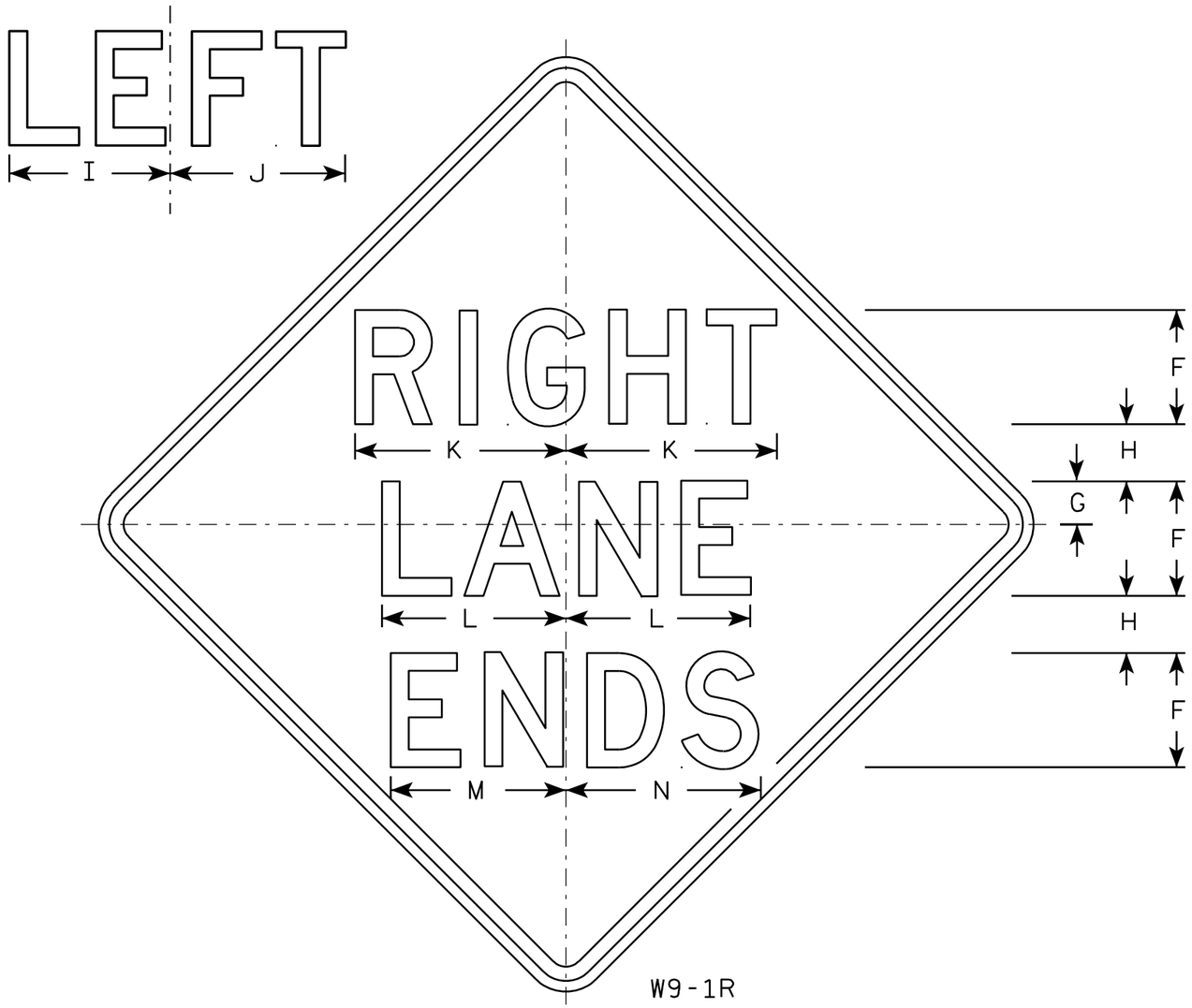
STANDARD SIGN
W8-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 03/14/13 PLATE NO. W8-52.8

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 137	E



W9-1R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. W9-1L same as W9-1R except the word Left replaces Right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	5	1 1/2	2 1/2	7 1/8	7 5/8	9 1/4	8 1/8	7 5/8	8 5/8													6.25
2S	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
2M	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
3	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
4	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
5	48		2 1/4	3/4	1	8	3	4	11 1/4	12 1/4	14 3/4	12 7/8	12 1/4	13 5/8													16.0

STANDARD SIGN
W9-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 03/18/13 PLATE NO. W9-1.8

PROJECT NO:

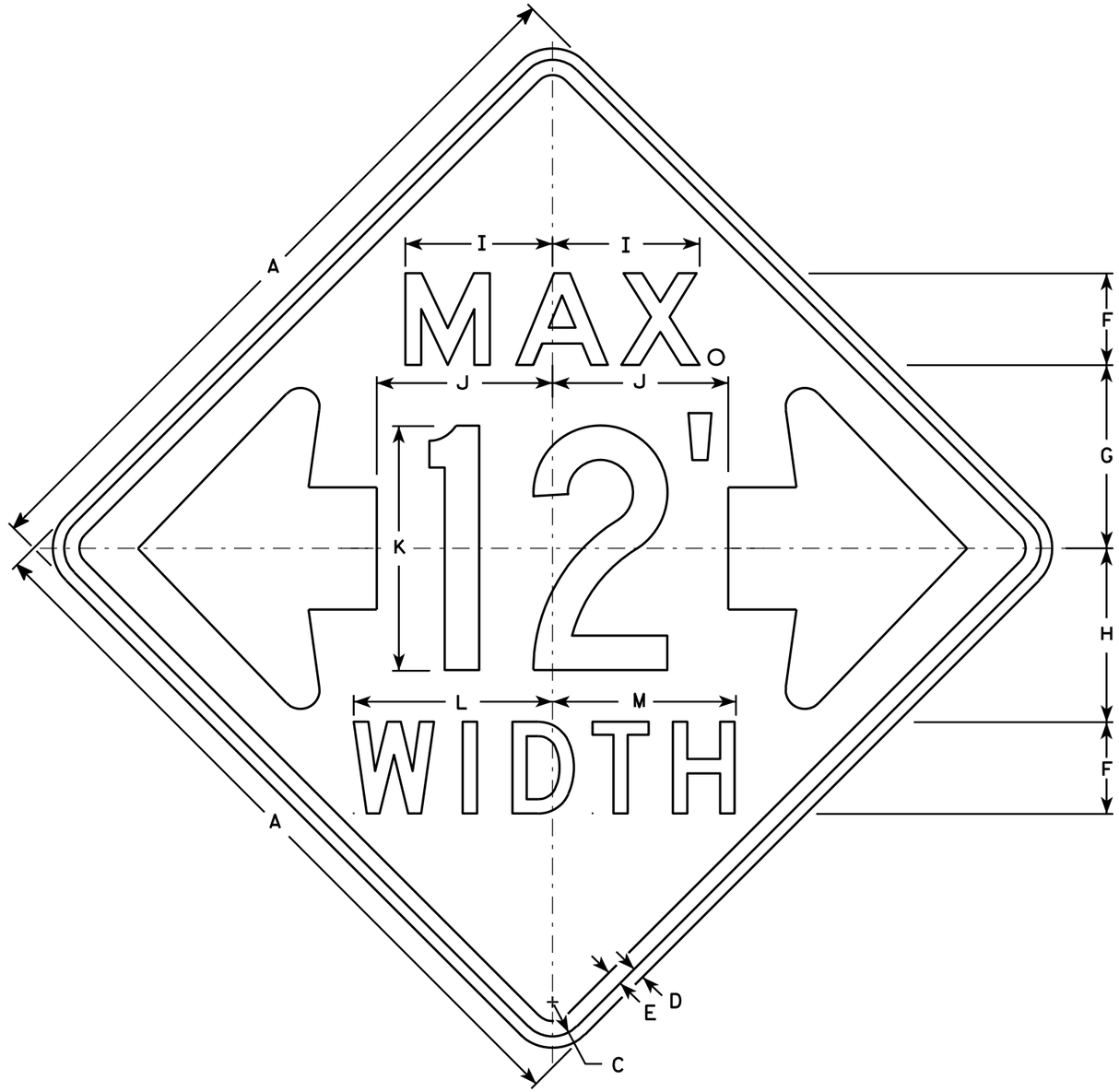
SHEET NO: 138 E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W91.DGN PLOT DATE : 18-MAR-2013 11:42 PLOT BY : mscsja

WISDOT/CADDS SHEET 42

7

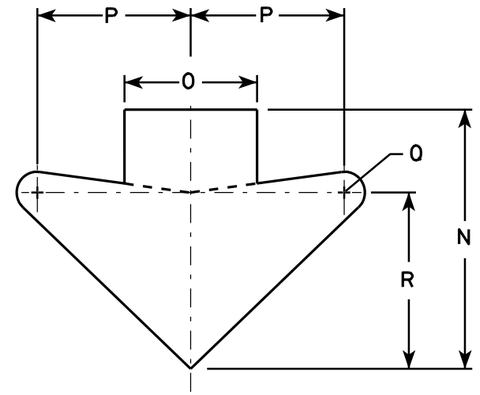
7



W12-52

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The top line is series E, the numerals are series C, and the bottom line is series D.
6. Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8									16.0
2M	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8									16.0
3																											
4																											
5																											

STANDARD SIGN
W12-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/16/11 PLATE NO. W12-52.7

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 139 | E

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W1252.DGN PLOT DATE : 16-MAR-2011 14:45 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.137199:1.000000

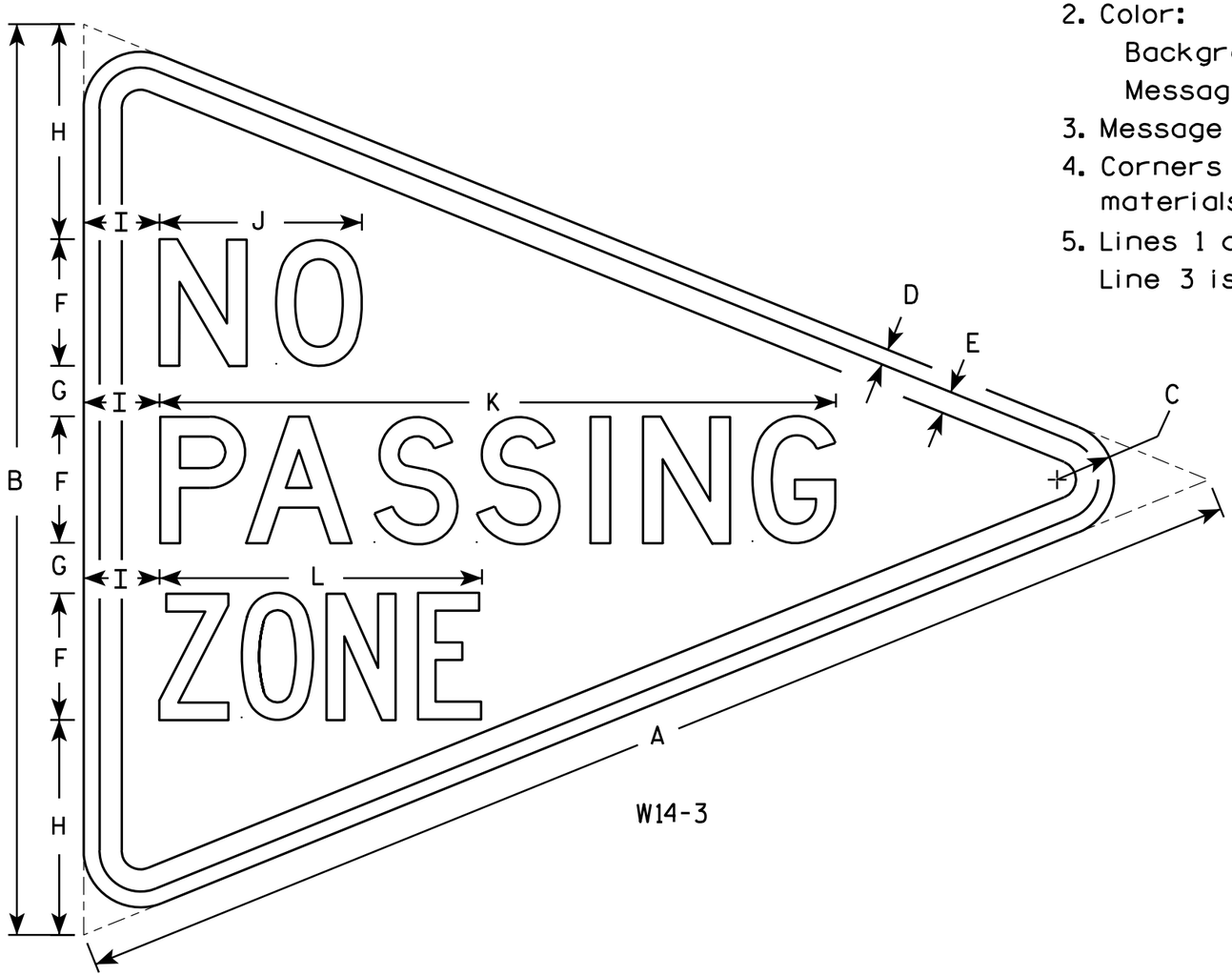
WISDOT/CADDS SHEET 42

7

7

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Message Series - See note 5
4. Corners and borders shall be rounded on all base materials for this sign.
5. Lines 1 and 2 are Series D.
   Line 3 is series C.



W14-3

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
2M	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
3	64	48	3	3/4	1 1/4	6	3	12	4	10 3/4	33 5/8	16 1/2															10.7
4																											
5																											

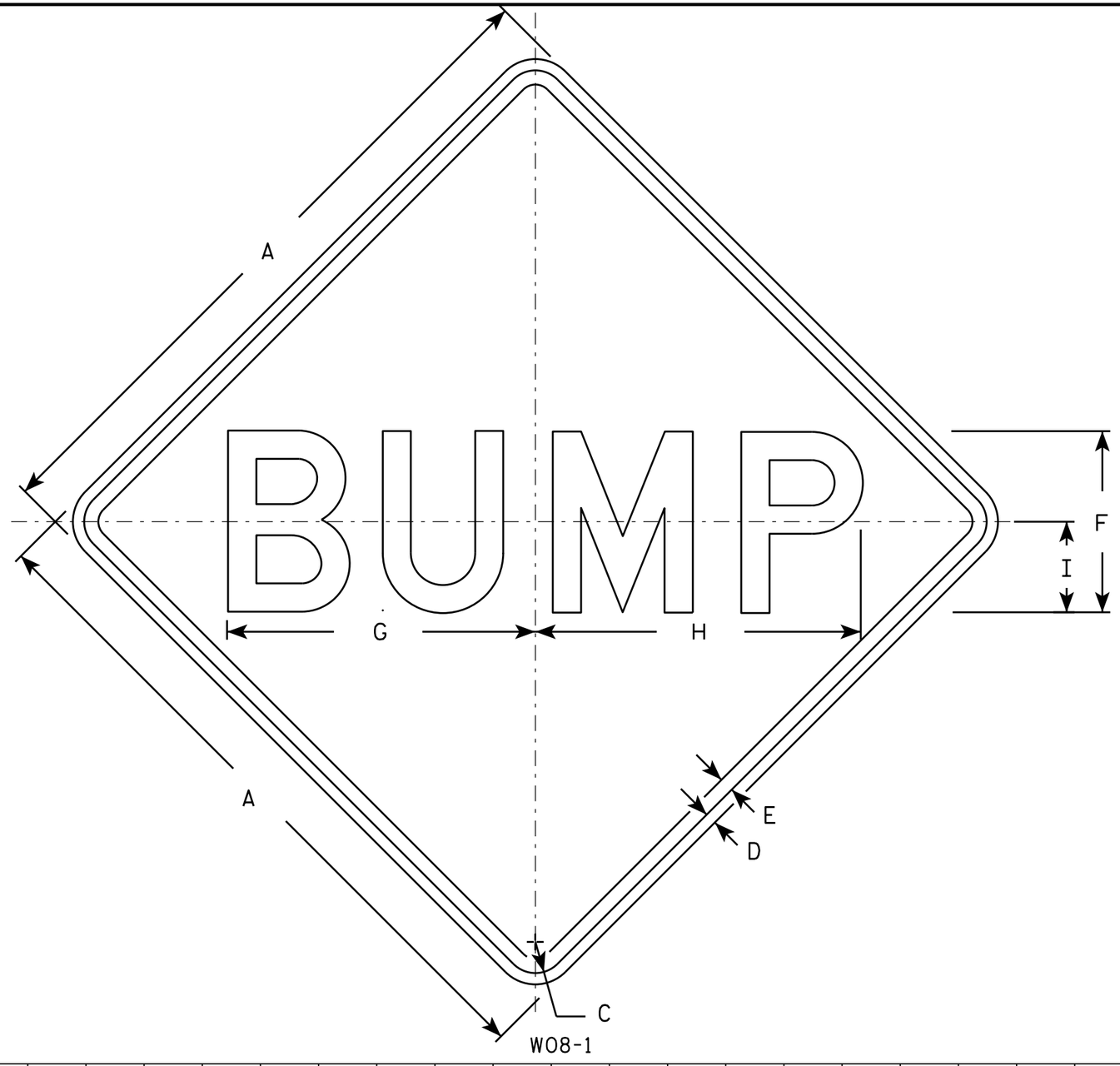
STANDARD SIGN
W14-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W14-3.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 140	E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	10	16 7/8	17 7/8	5																		9.0
2S	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
2M	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
3	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
4	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0
5	48		2 1/4	3/4	1	12	20 3/8	21 5/8	6																		16.0

STANDARD SIGN
W08-1

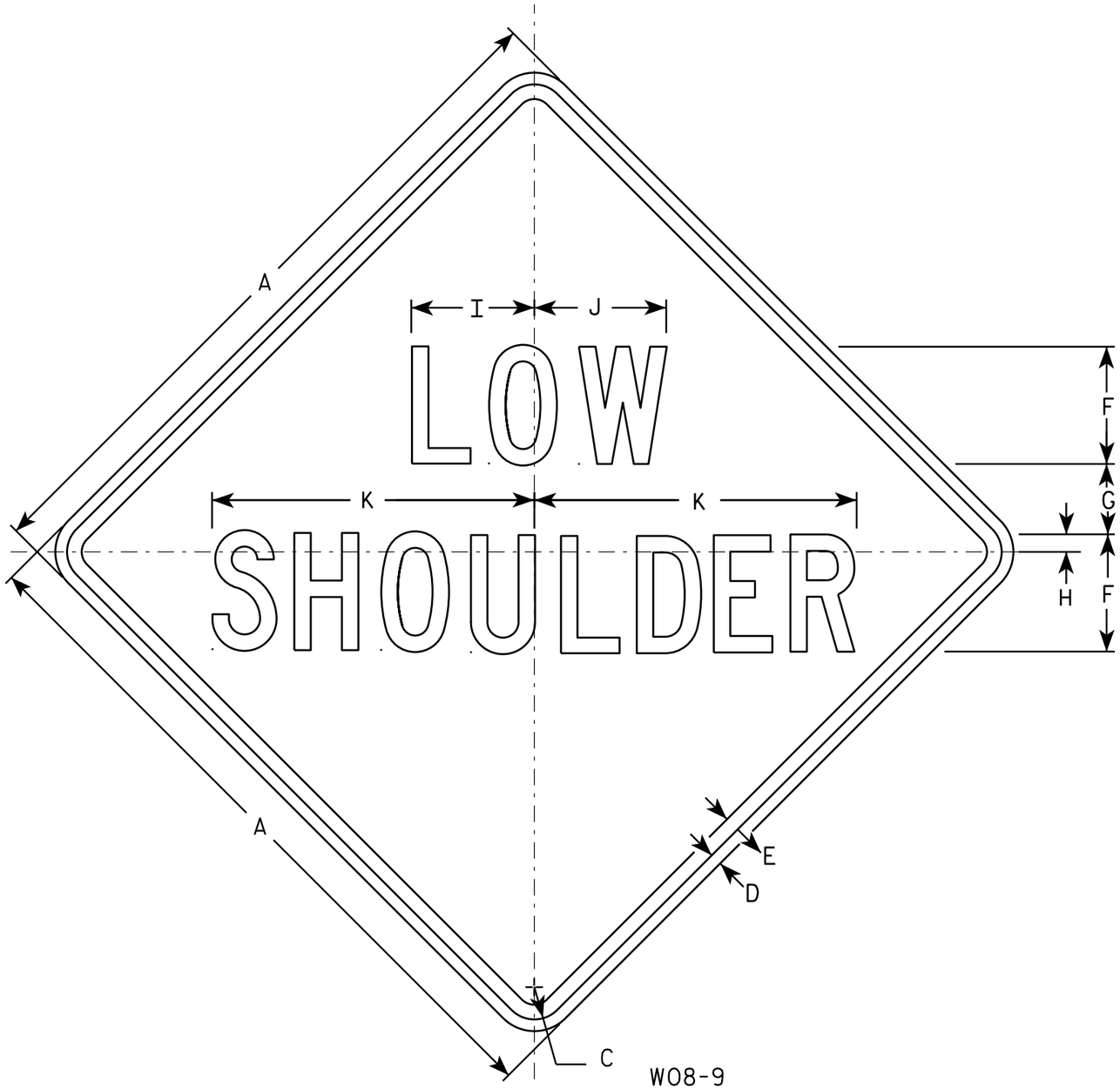
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE 141 W08-1.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W081.DGN PLOT DATE : 20-NOV-2013 12:24 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 6.688833:1.000000 WISDOT/CADDS SHEET 42



W08-9

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	3 1/2	1	6 1/4	6 3/4	16 1/2																9.0
2S	48		2 1/4	3/4	1	8	5	1 1/4	8 1/4	9	21 7/8																16.0
2M	48		2 1/4	3/4	1	8	5	1 1/4	8 1/4	9	21 7/8																16.0
3	48		2 1/4	3/4	1	8	5	1 1/4	8 1/4	9	21 7/8																16.0
4	48		2 1/4	3/4	1	8	5	1 1/4	8 1/4	9	21 7/8																16.0
5	48		2 1/4	3/4	1	8	5	1 1/4	8 1/4	9	21 7/8																16.0

STANDARD SIGN
W08-9

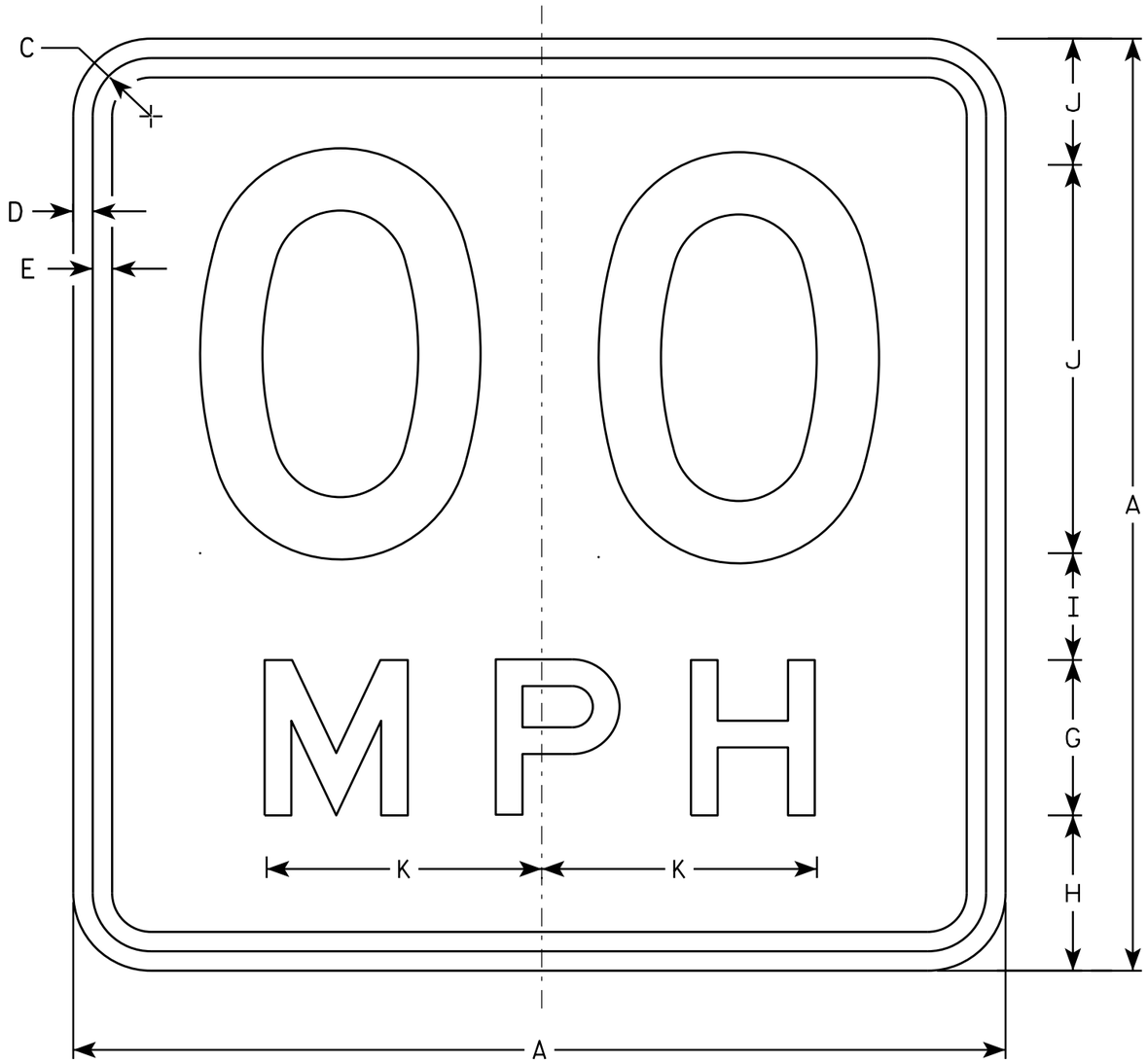
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE W08-9.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W089.DGN PLOT DATE : 20-NOV-2013 14:06 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 6.755110:1.000000
WISDOT/CADDS SHEET 42



W013-1

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
   Line 2 is Series E

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	7 1/8																4.00
2S	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
2M	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
3	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

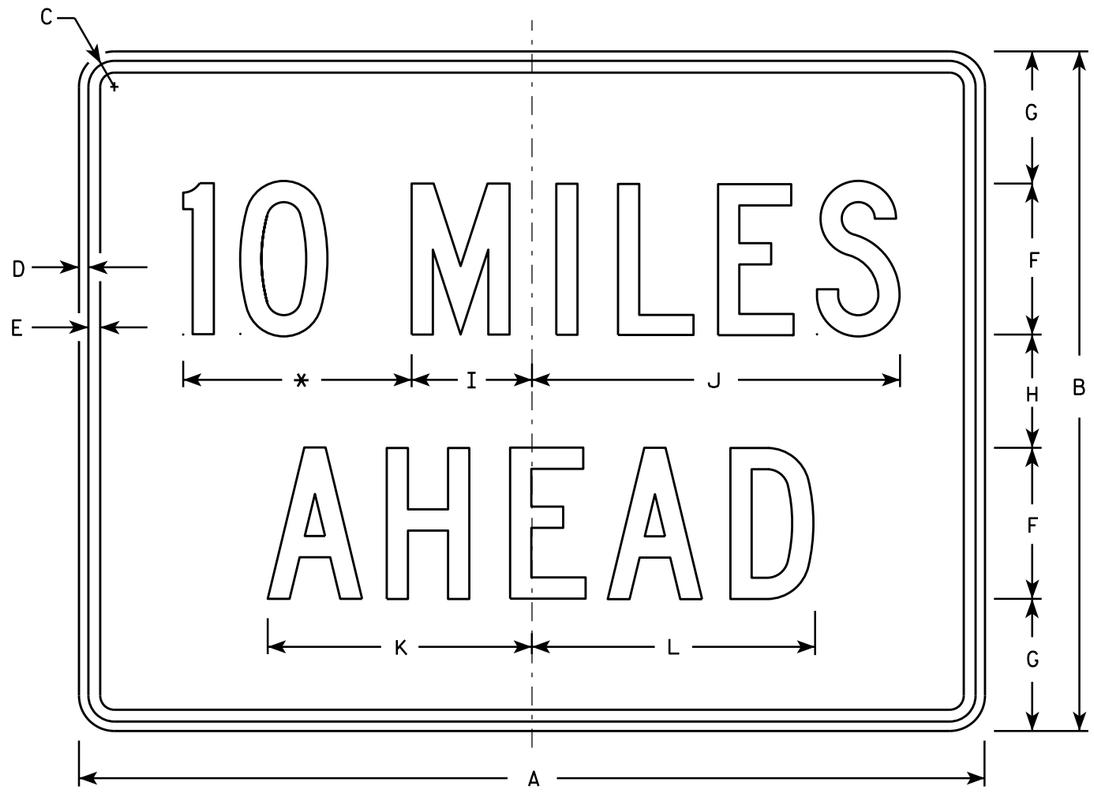
STANDARD SIGN
W013-1

WISCONSIN DEPT OF TRANSPORTATION

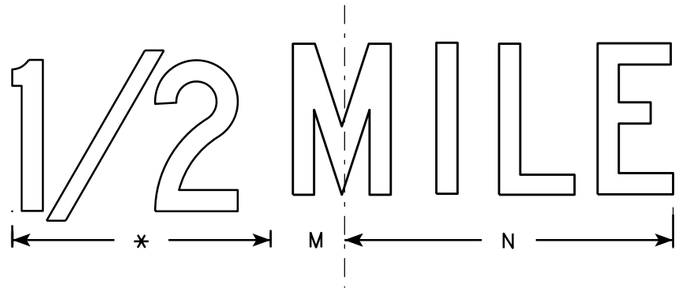
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/21/13 PLATE 143 W013-1.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



W057-52



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

* See note 5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	24	1 1/8	3/8	1/2	6	4 1/2	3	4 3/4	14 5/8	10 5/8	11 3/8	2	13													6.0
2S	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0
2M	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0
3	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0
4	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0
5	48	36	1 3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0

STANDARD SIGN
W057-52

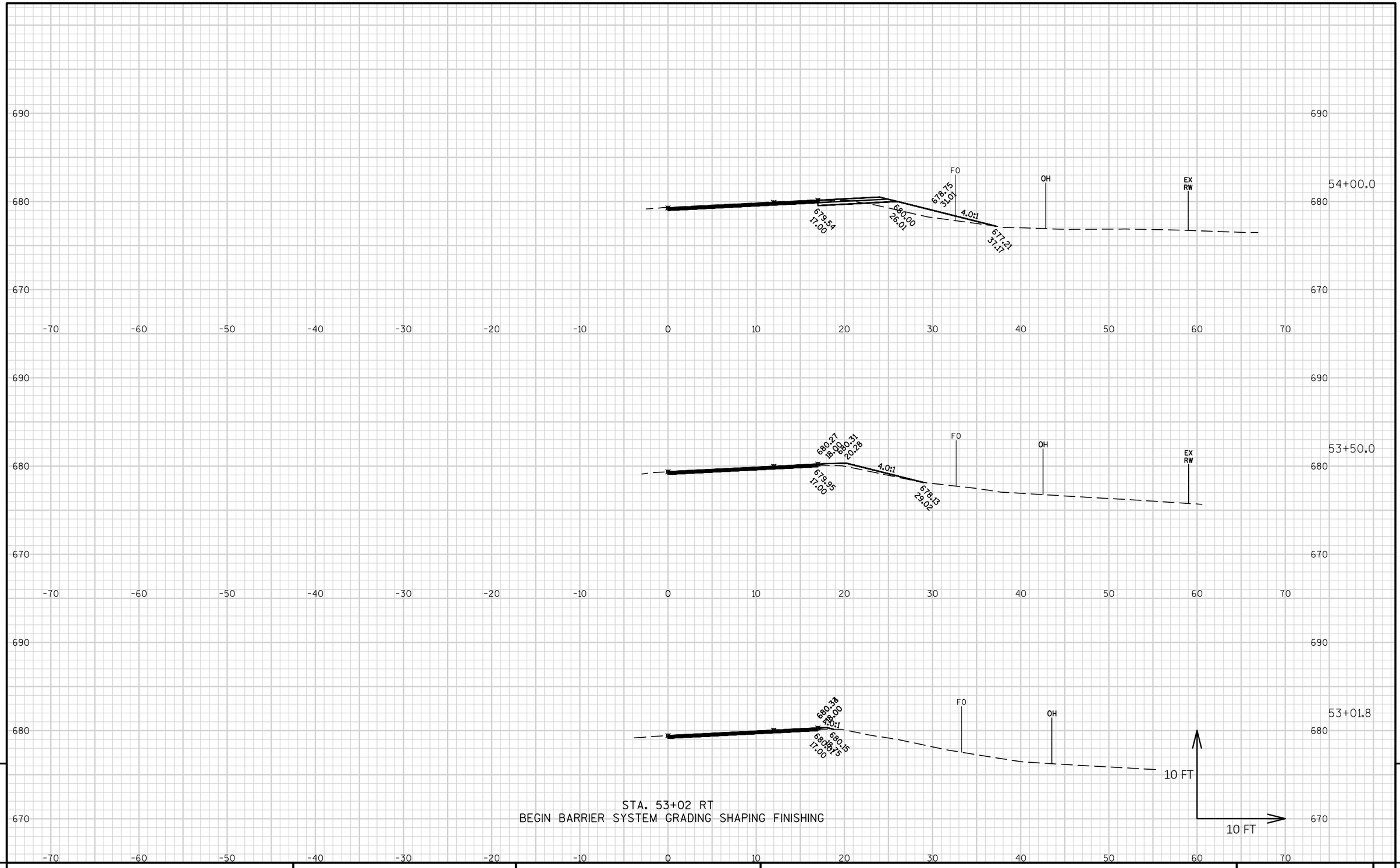
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE W057-52.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W05752.DGN PLOT DATE : 28-FEB-2014 11:42 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 8.026599:1.000000 WISDOT/CADDS SHEET 42

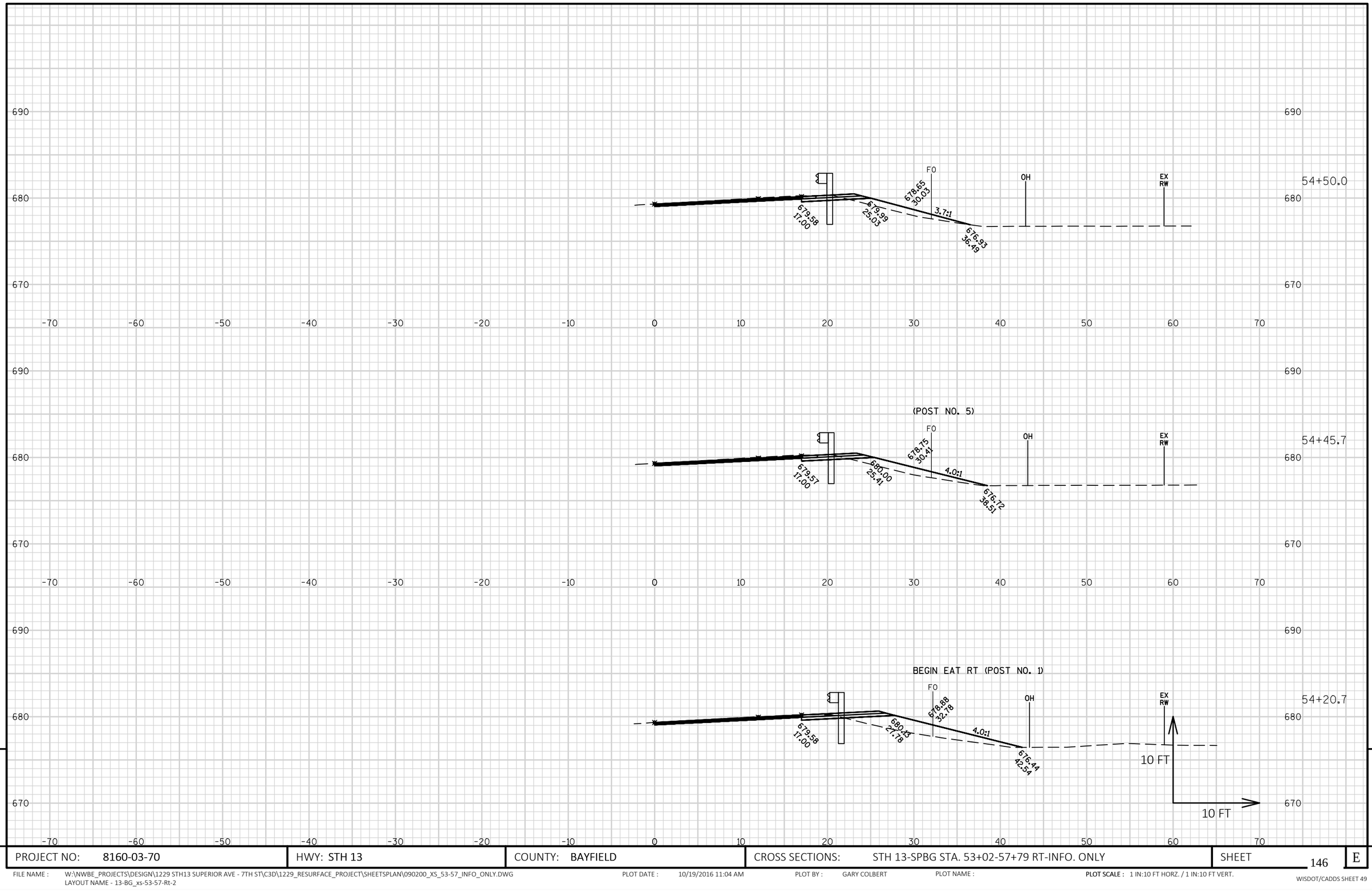


STA. 53+02 RT
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING

9 9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 53+02-57+79 RT-INFO. ONLY	SHEET 145	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200_XS_53-57_INFO_ONLY.DWG
LAYOUT NAME - 13-BG_xs-53-57-Rt-1
PLOT DATE : 10/19/2016 11:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

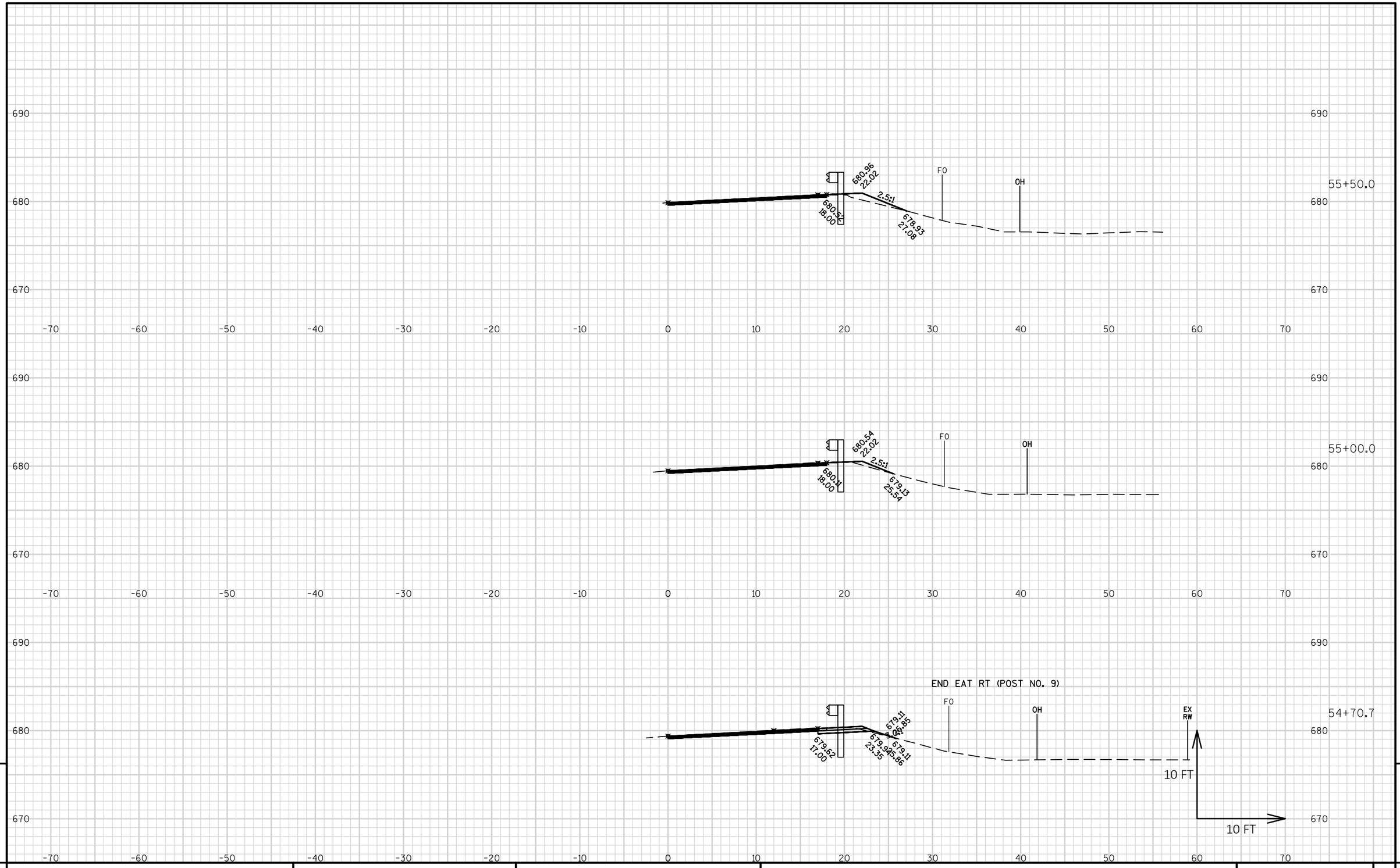


PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 53+02-57+79 RT-INFO. ONLY	SHEET 146	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200_XS_53-57_INFO_ONLY.DWG
LAYOUT NAME - 13-BG_xs-53-57-Rt-2

PLOT DATE : 10/19/2016 11:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



9

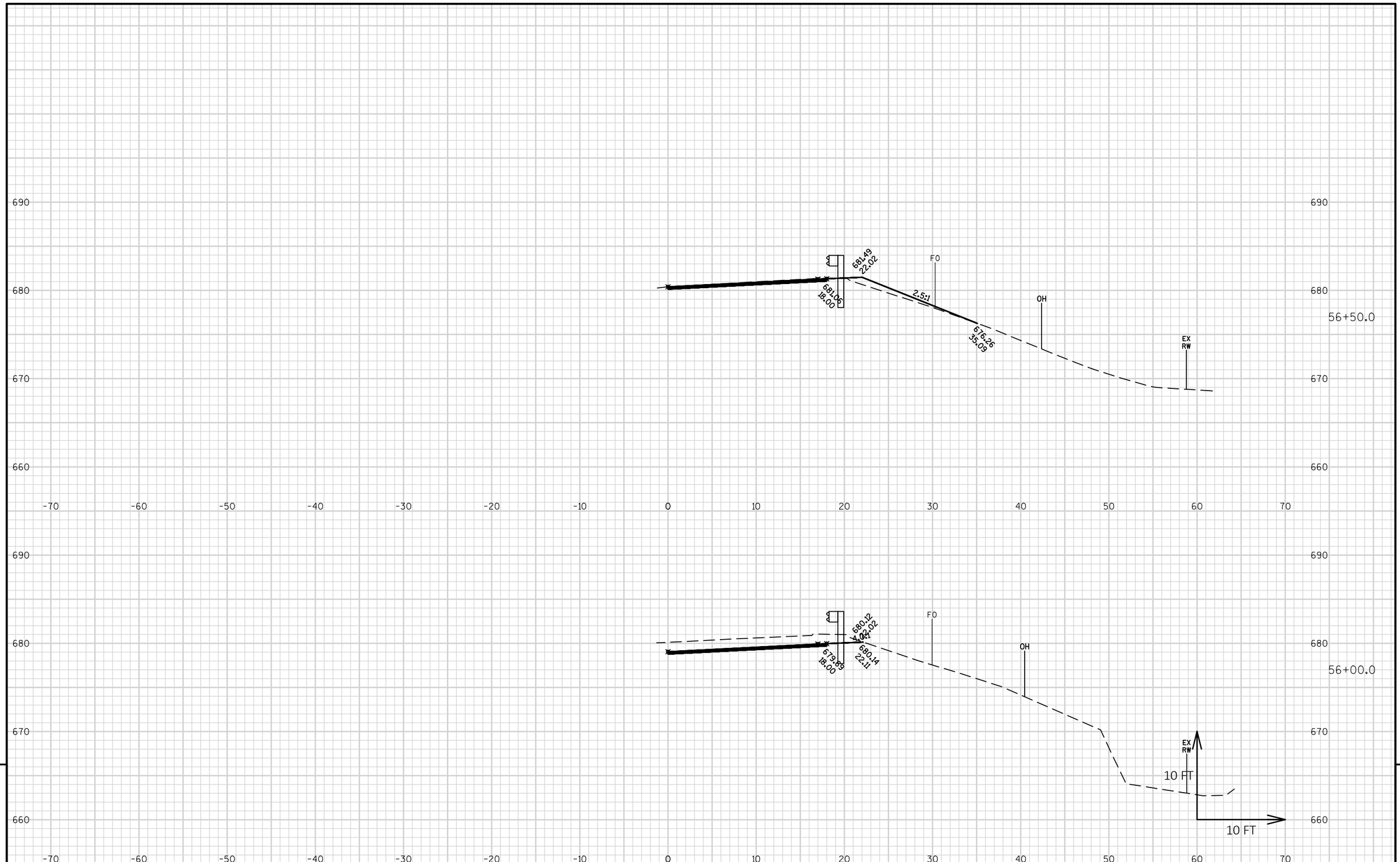
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 53+02-57+79 RT-INFO. ONLY	SHEET 147	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200_XS_53-57_INFO_ONLY.DWG
LAYOUT NAME - 13-BG_xs-53-57-Rt-3

PLOT DATE : 10/19/2016 11:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 53+02-57+79 RT-INFO. ONLY	SHEET 148	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200_XS_53-57_INFO_ONLY.DWG
LAYOUT NAME - 13-BG_xs-53-57-Rt-4

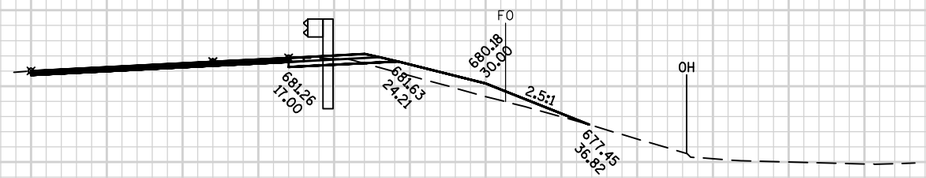
PLOT DATE : 10/19/2016 11:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

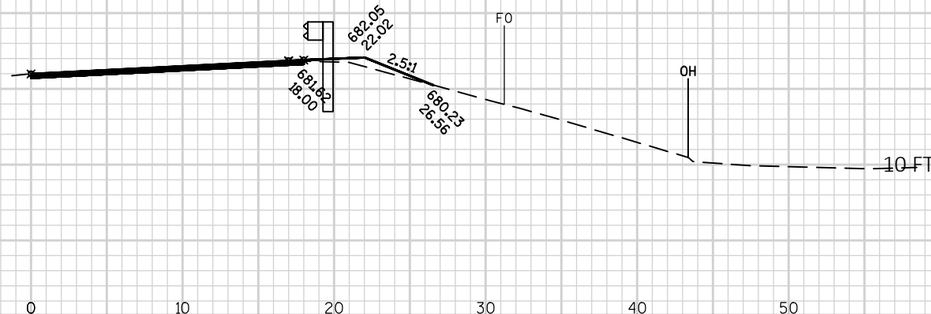
(POST NO. 5)

57+26.9

BEGIN EAT RT (POST NO. 9)



57+01.9



57+00.0

9

9

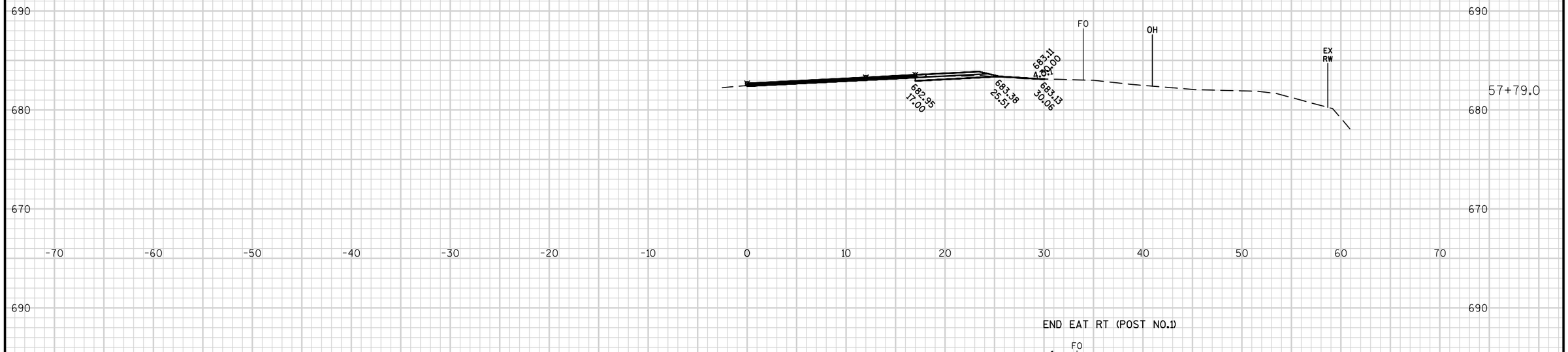
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 53+02-57+79 RT-INFO. ONLY	SHEET 149	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200_XS_53-57_INFO_ONLY.DWG
LAYOUT NAME - 13-BG_xs-53-57-Rt-5

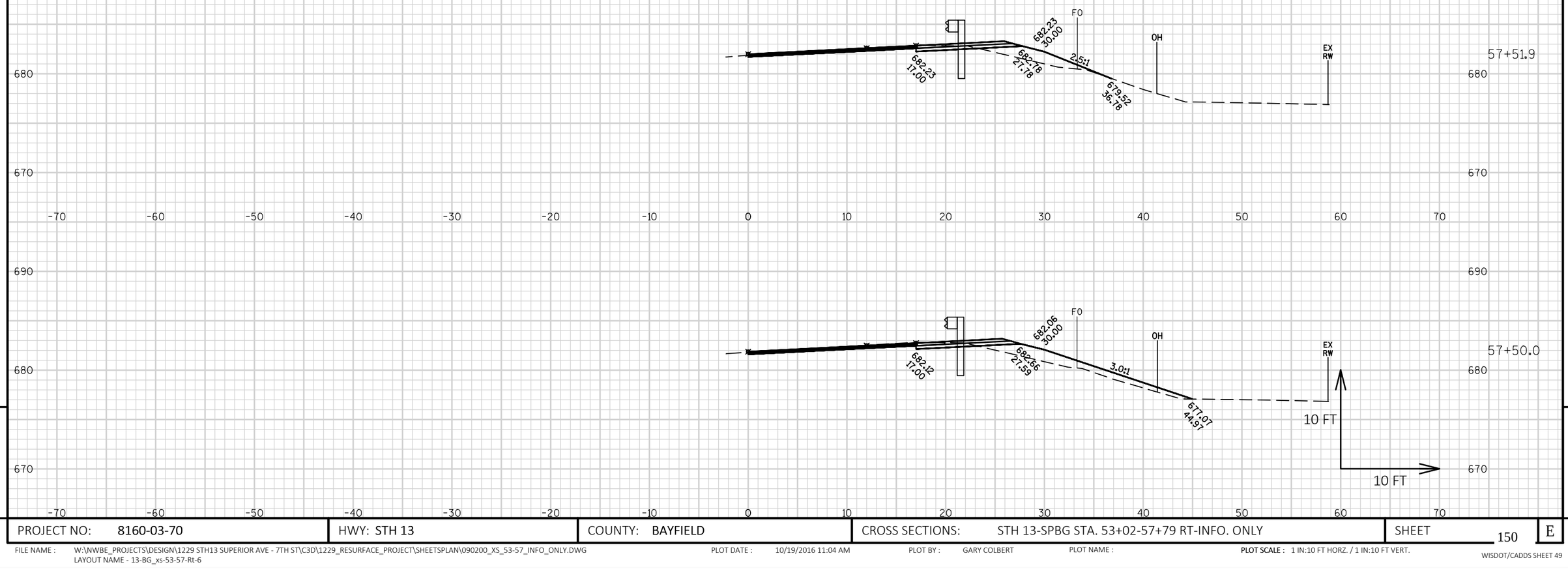
PLOT DATE : 10/19/2016 11:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

STA. 57+79 RT
END BARRIER SYSTEM GRADING SHAPING FINISHING



END EAT RT (POST NO.1)



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 53+02-57+79 RT-INFO. ONLY	SHEET 150	E

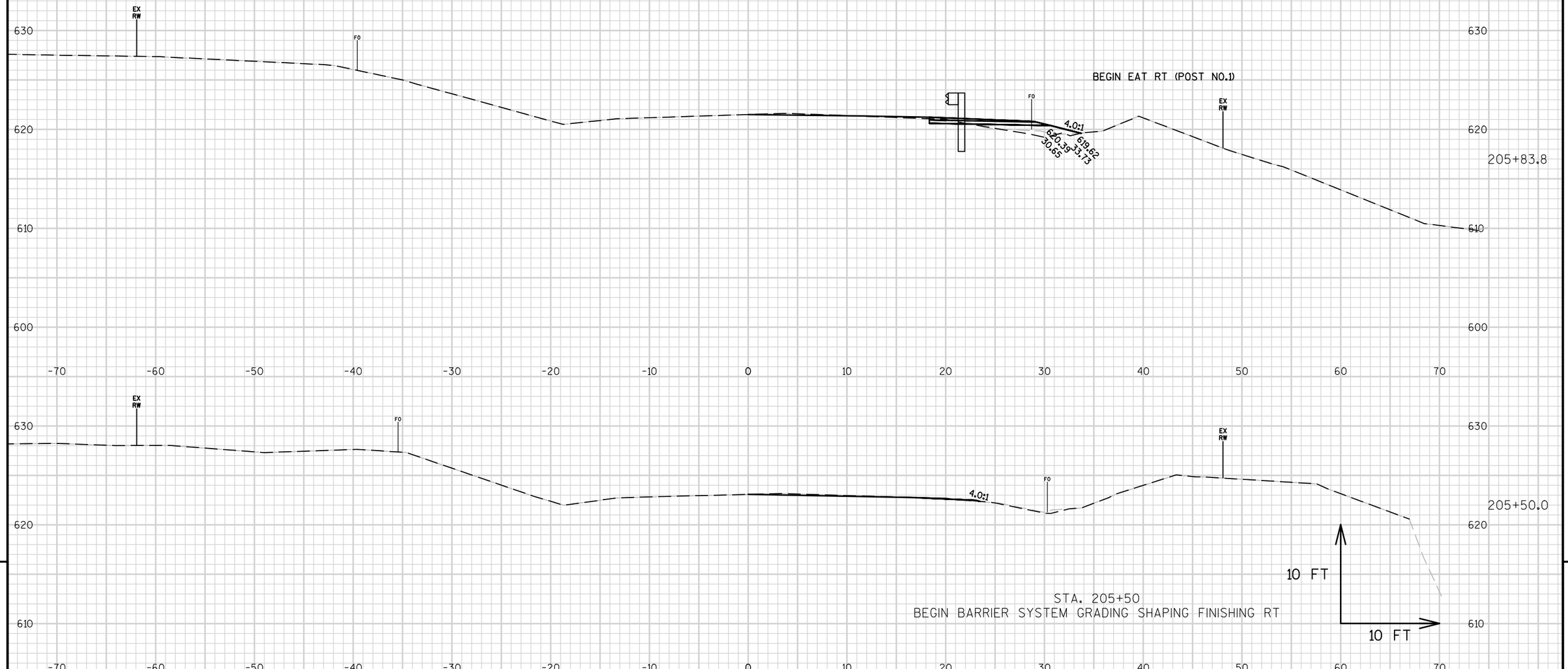
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200_XS_53-57_INFO_ONLY.DWG
LAYOUT NAME - 13-BG_xs-53-57-Rt-6
PLOT DATE : 10/19/2016 11:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.

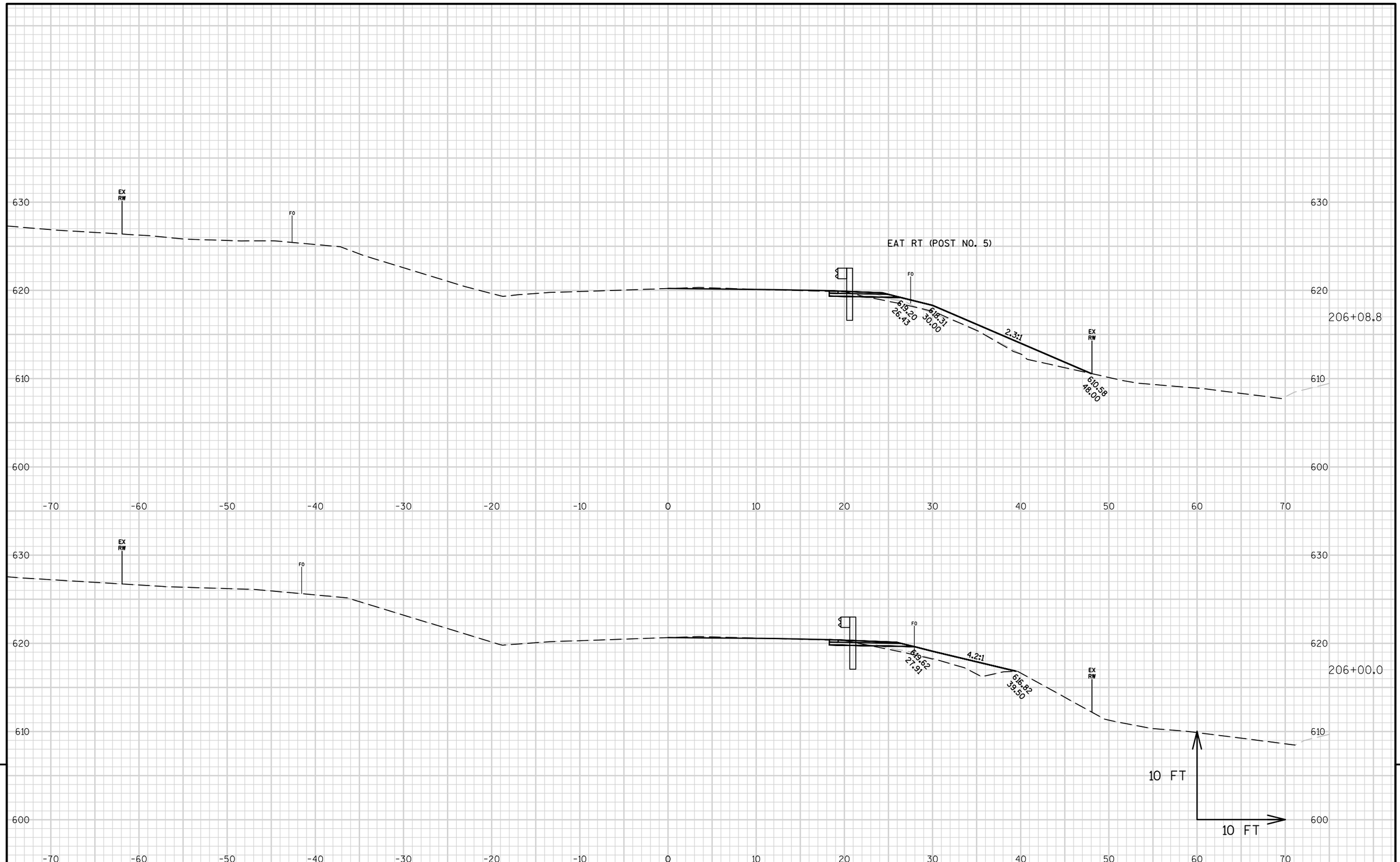


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ORL.DWG
LAYOUT NAME - 1_XS-MGS 208 RT
PLOTDATE : 1/17/2017 8:44 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET 152	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ONLY.DWG
LAYOUT NAME - 2_XS-MGS 208 RT
PLOTTED DATE : 1/17/2017 8:41 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

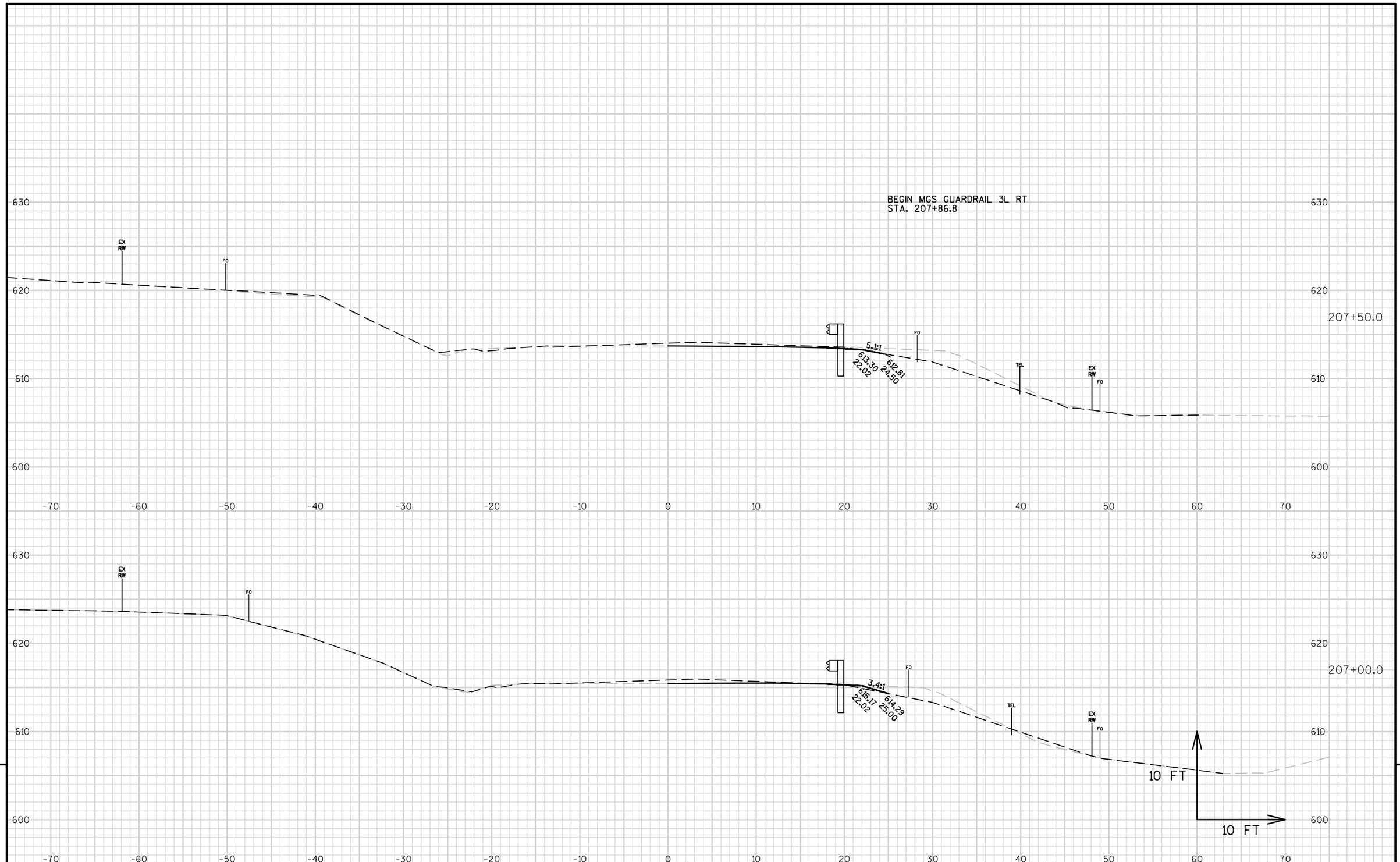
WISDOT/CADDS SHEET 49

9

9

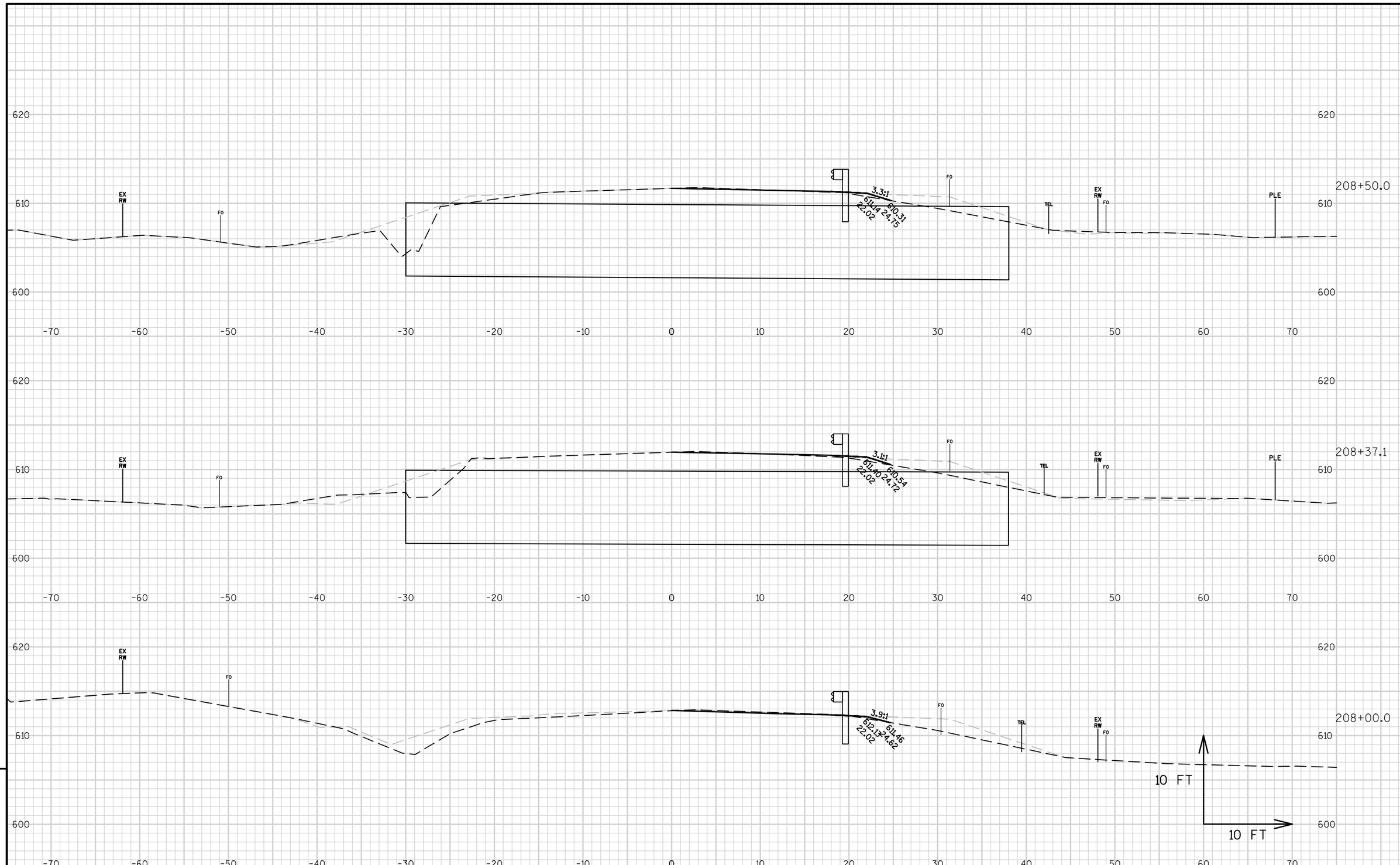
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET 153	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ONLY.DWG PLOT DATE : 1/17/2017 8:41 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 3_XS-MGS 208 RT



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET 154 E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ONLY.DWG
LAYOUT NAME - 4_XS-MGS 208 RT
PLOT DATE : 1/17/2017 8:41 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

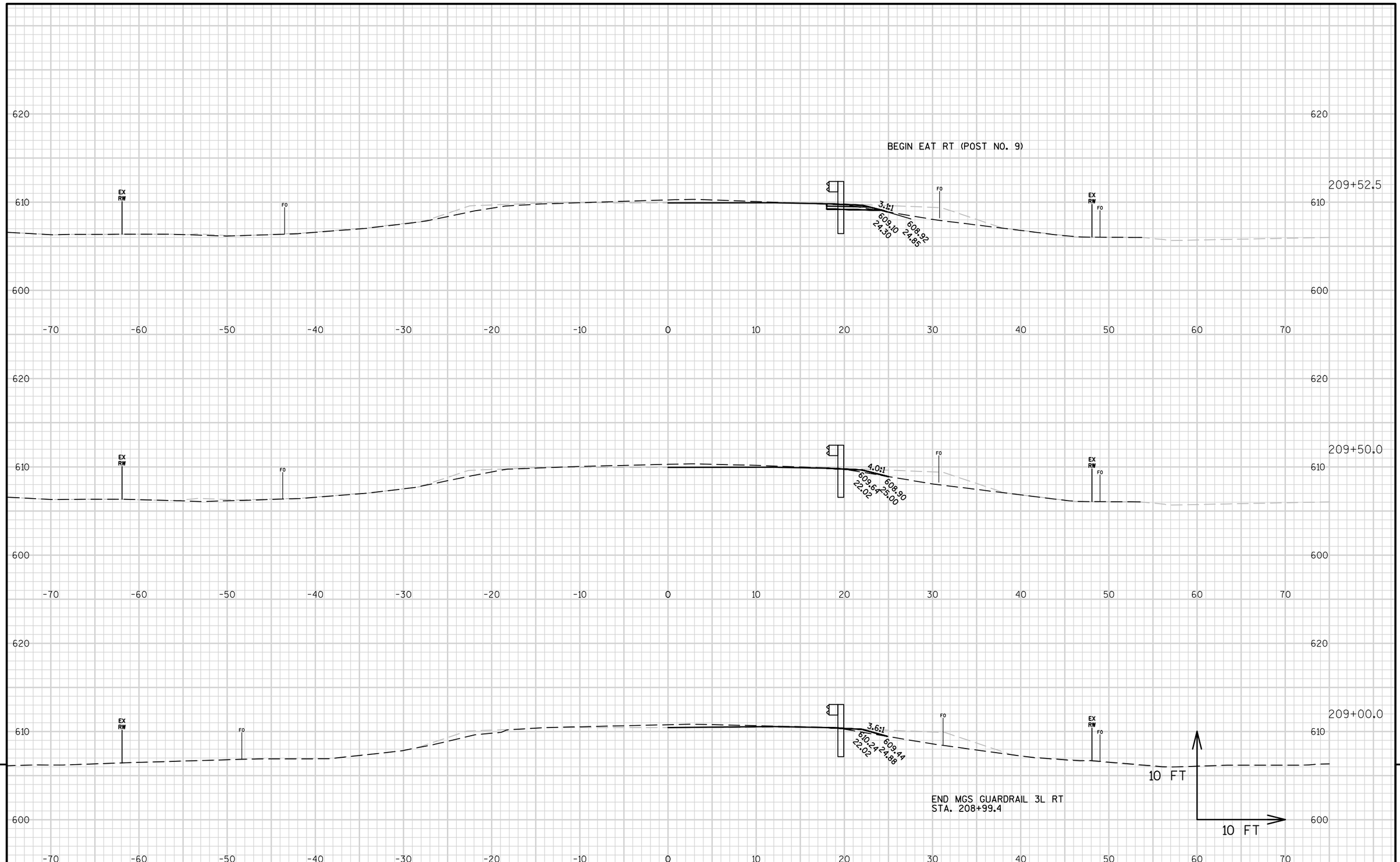


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET 155	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ORIG.DWG
LAYOUT NAME - 5_XS-MGS 208 RT
PLOTTED : DATE : 1/17/2017 8:41 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



9

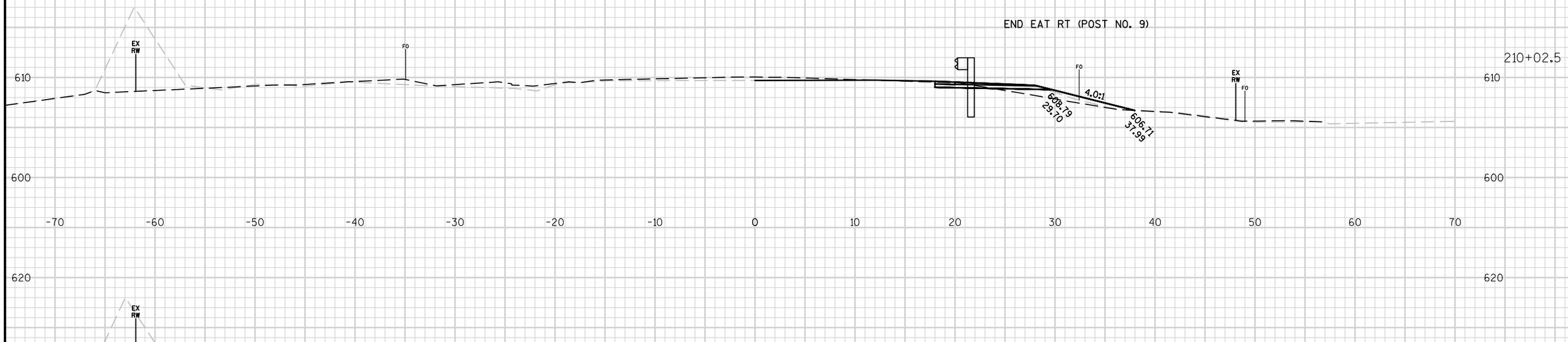
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET 156	E

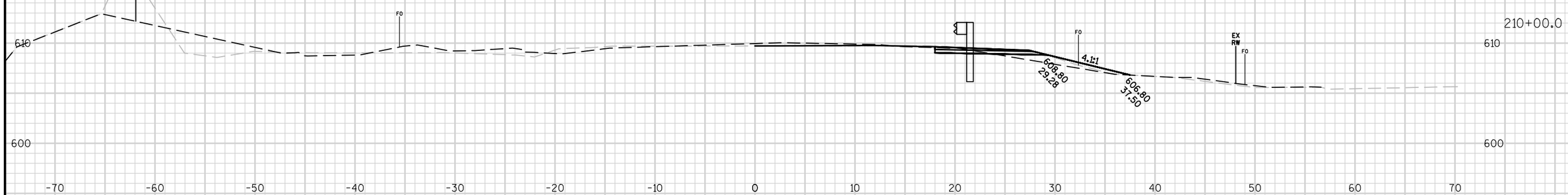
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ONLY.DWG
LAYOUT NAME - 6_XS-MGS 208 RT
PLOT DATE : 1/17/2017 8:41 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

END EAT RT (POST NO. 9)

210+02.5

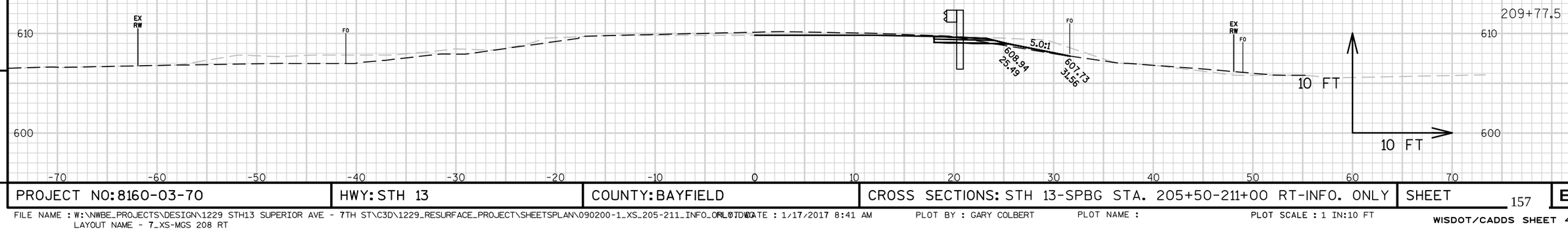


210+00.0



EAT RT (POST NO. 9)

209+77.5



9

9

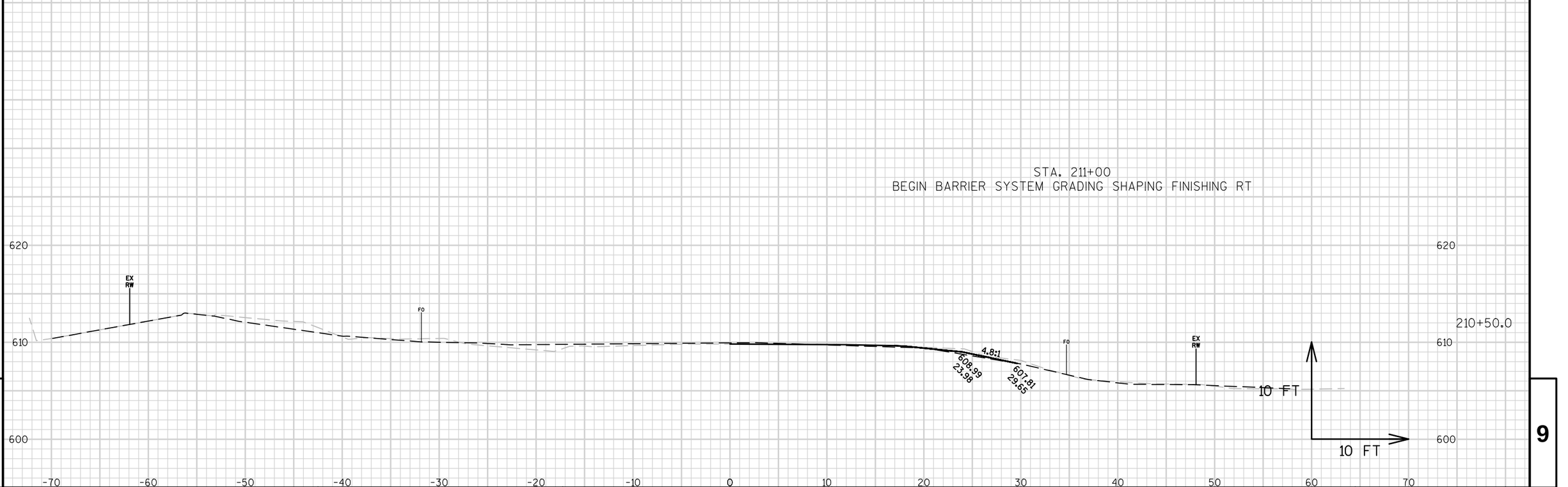
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET 157	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ORL.DWG
LAYOUT NAME - 7_XS-MGS 208 RT

PLOT DATE : 1/17/2017 8:41 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

STA. 211+00
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING RT

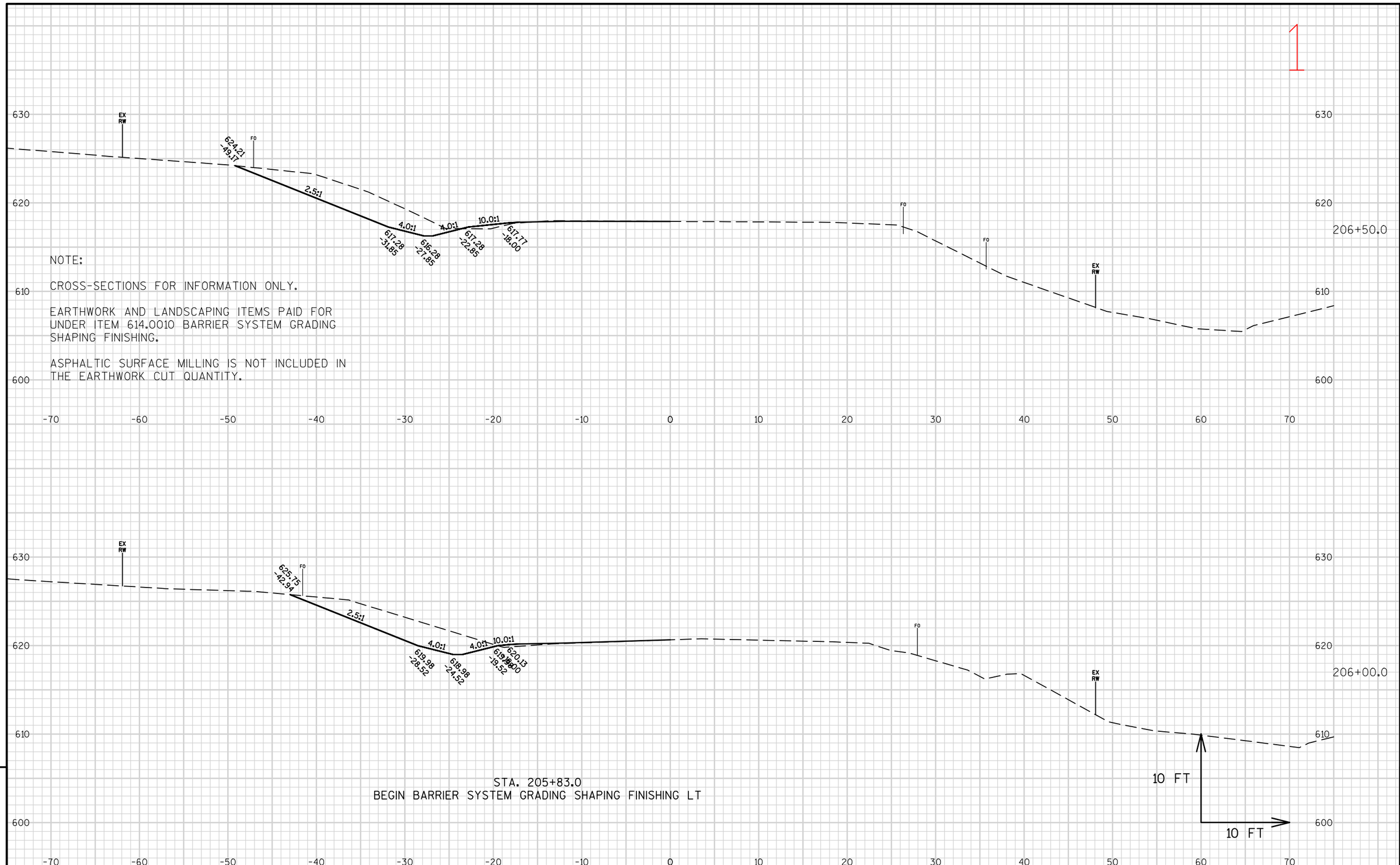


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+50-211+00 RT-INFO. ONLY	SHEET 158	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-211_INFO_ONLY.DWG PLOT DATE : 1/17/2017 8:41 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 8_XS-MGS 208 RT



NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.

STA. 205+83.0
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING LT

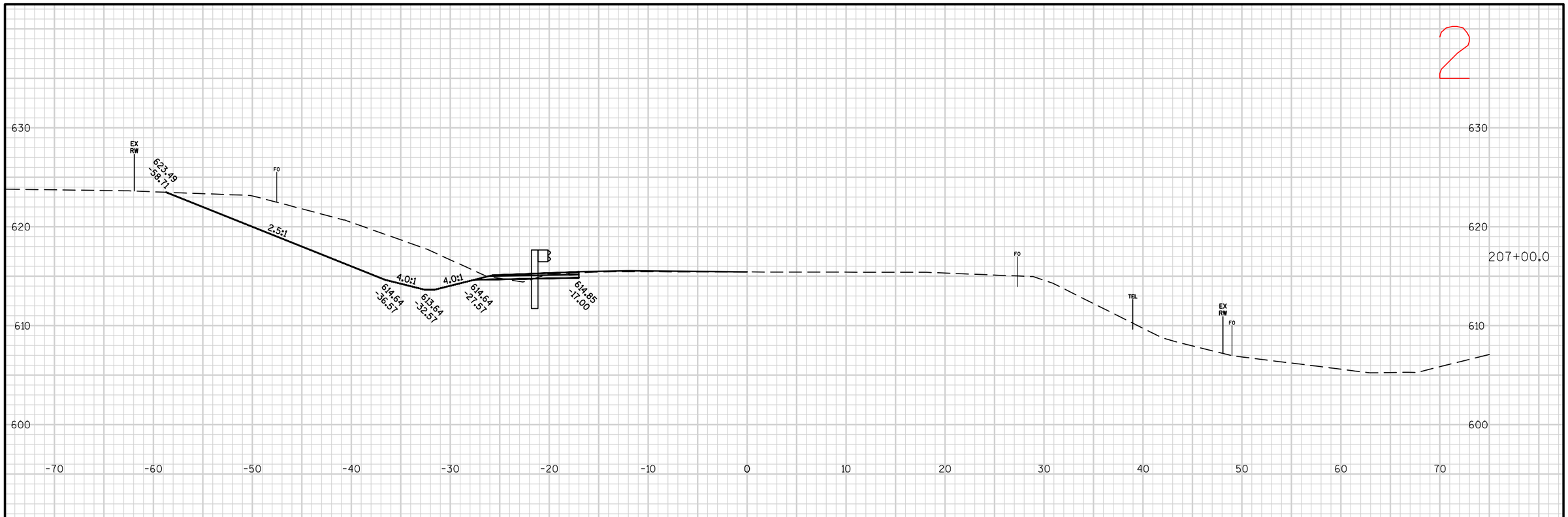
9

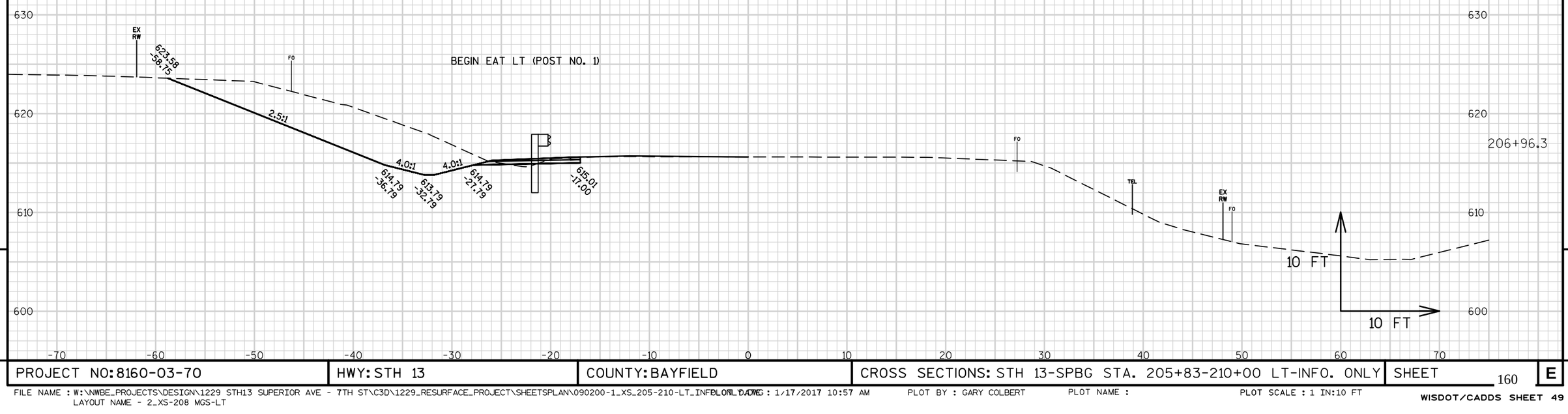
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 159	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-210-LT_INFO_ONLY.DWG PLOT DATE : 1/17/2017 10:57 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 1_XS-208 MGS-LT

2





9

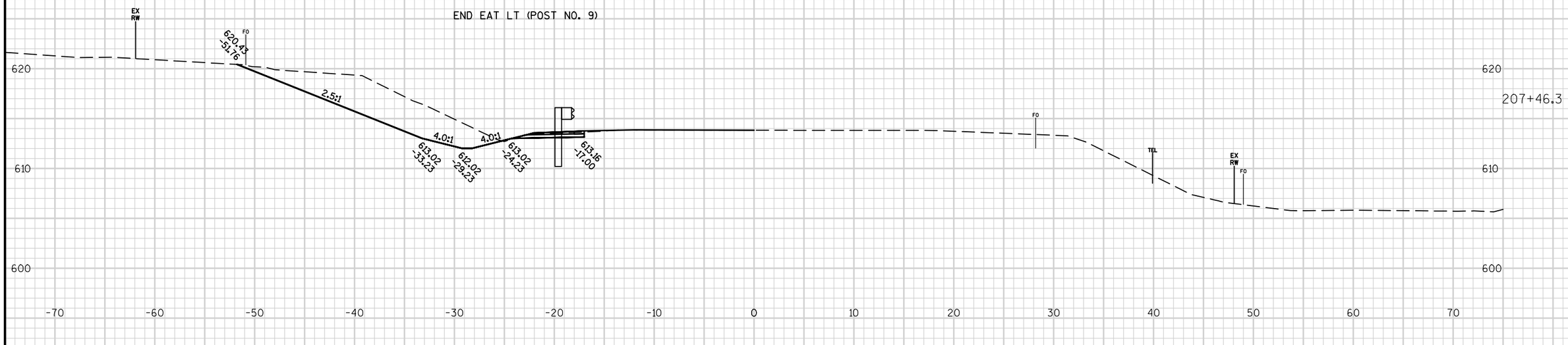
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 160	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-210-LT_INFO_ONLY.DWG PLOT DATE : 1/17/2017 10:57 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 2_XS-208 MGS-LT

3

END EAT LT (POST NO. 9)



EAT LT (POST NO. 5)

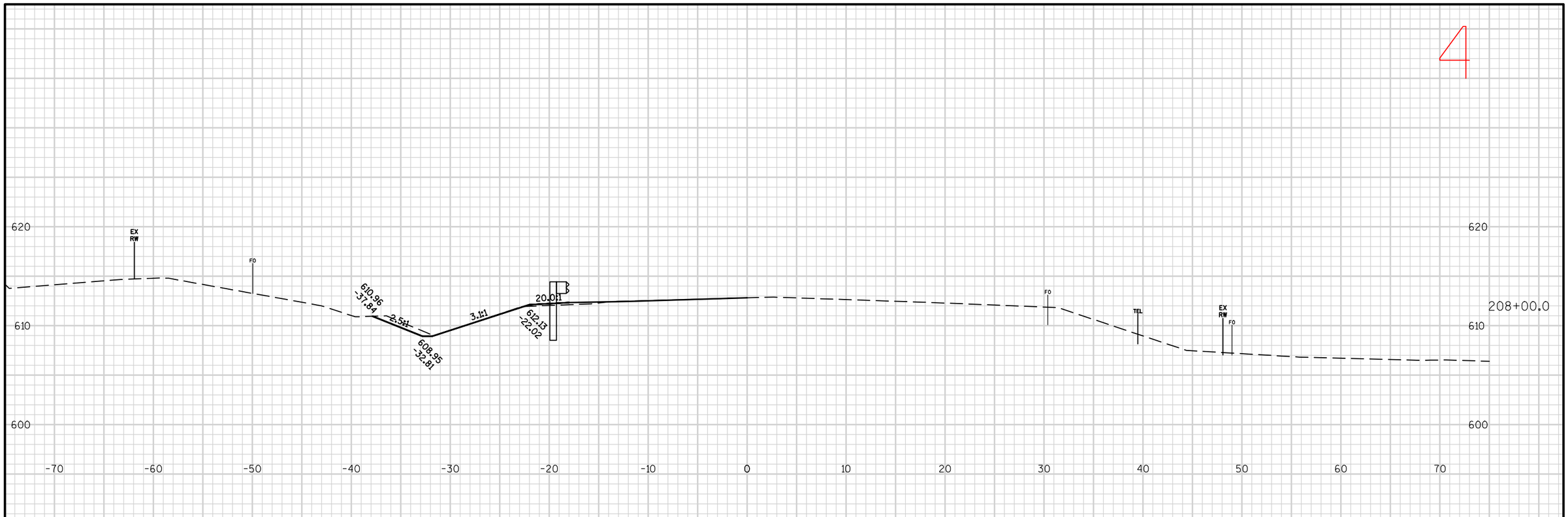
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 161	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-210-LT_INFO_ONLY.DWG PLOT DATE : 1/17/2017 10:57 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 3_XS-208 MGS-LT

WISDOT/CADDS SHEET 49



9

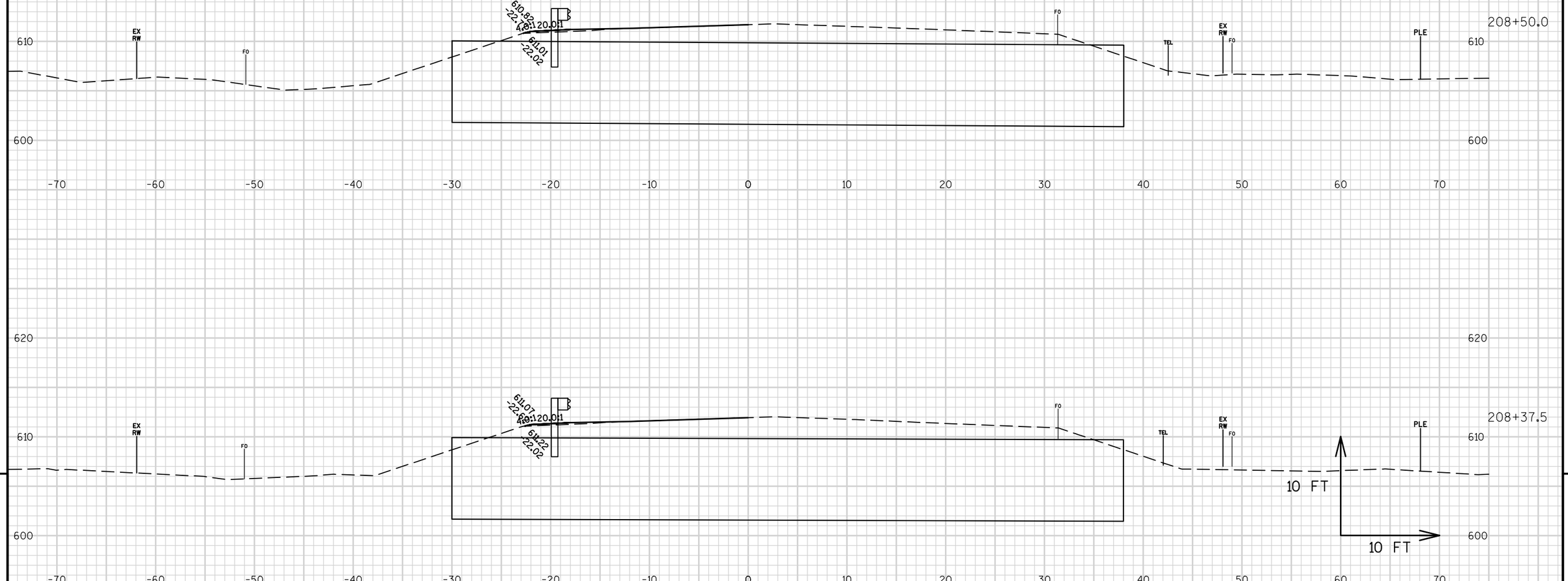
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 162	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-210-LT_INFO_ONLY.DWG
LAYOUT NAME - 4_XS-208 MGS-LT
PLOTTED : 1/17/2017 10:57 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

5

END MGS GUARDRAIL 3L
STA. 208+99.4 LT



9

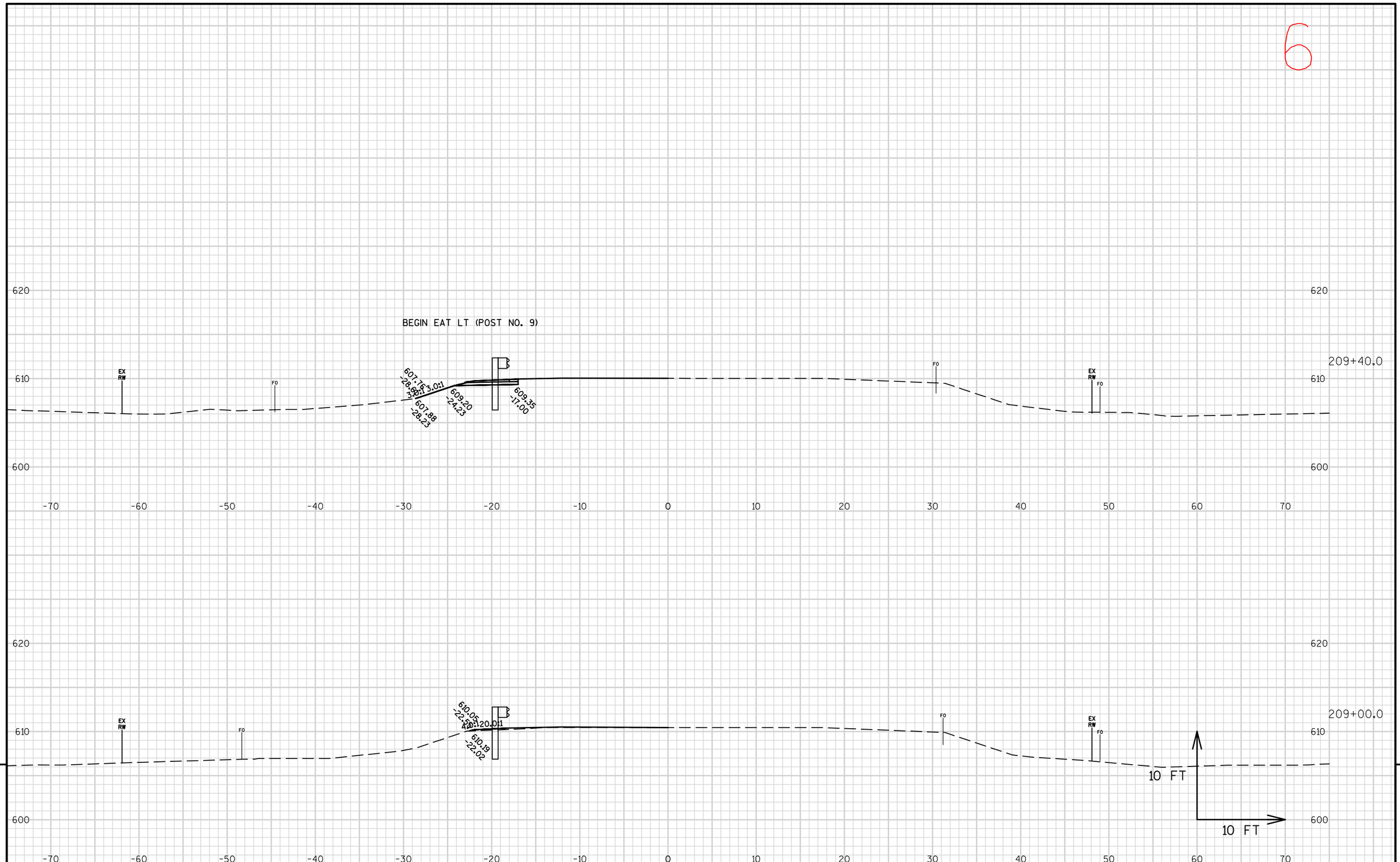
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 163	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-210-LT_INFO_ONLY.DWG PLOT DATE : 1/17/2017 10:57 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 5_XS-208 MGS-LT

WISDOT/CADDS SHEET 49

6



9

9

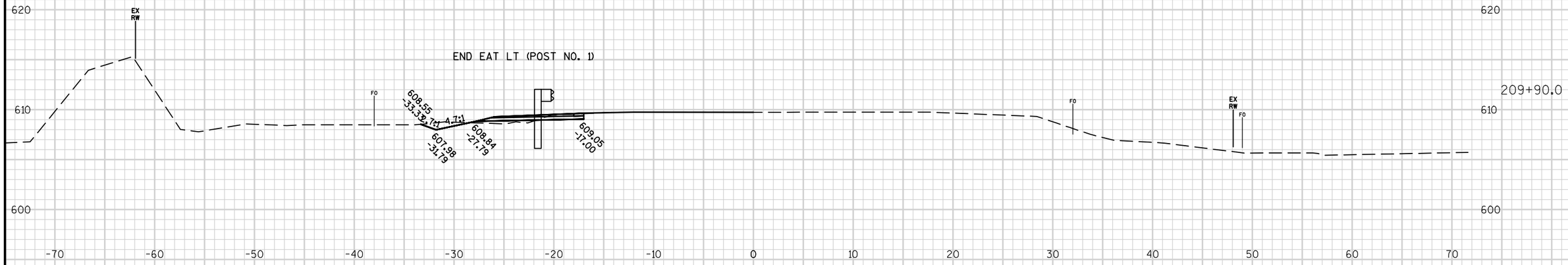
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 164	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-210-LT_INFO_ONLY.DWG PLOT DATE : 1/17/2017 10:57 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 6_XS-208 MGS-LT

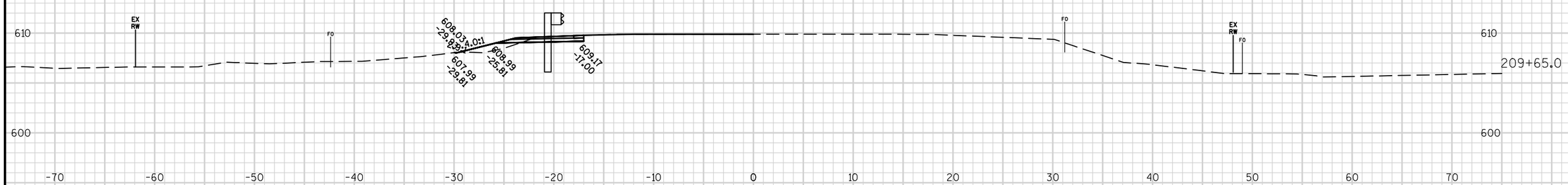
WISDOT/CADDS SHEET 49

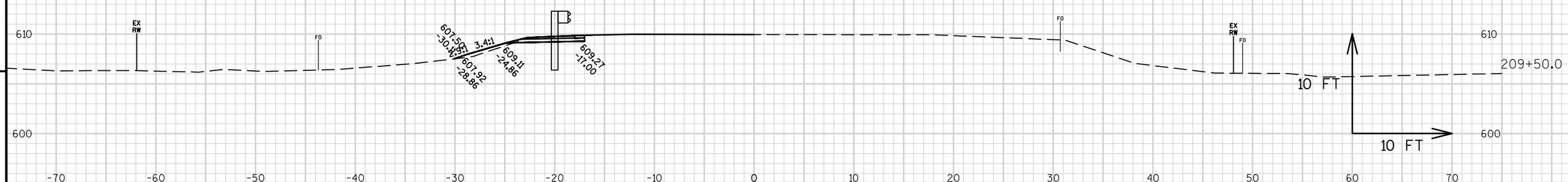
7

END EAT LT (POST NO. 1)



EAT LT (POST NO. 50





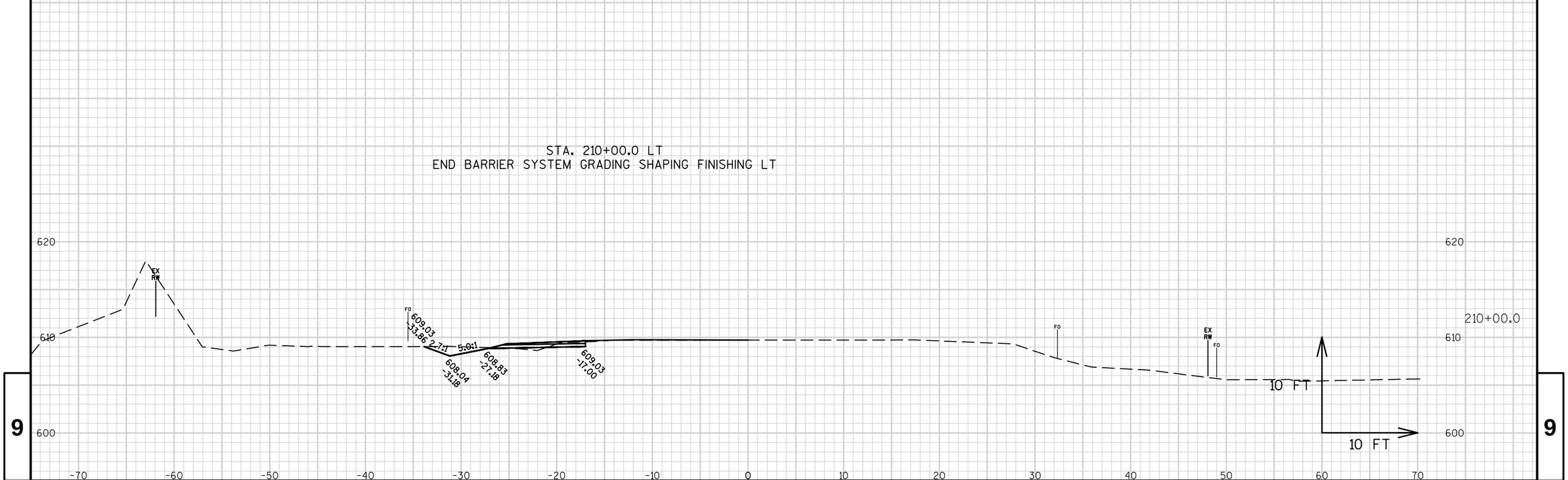
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 165	E

8

STA. 210+00.0 LT
END BARRIER SYSTEM GRADING SHAPING FINISHING LT



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 205+83-210+00 LT-INFO. ONLY	SHEET 166	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090200-1_XS_205-210-LT_INFO_ONLY.DWG PLOT DATE : 1/17/2017 10:58 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 8_XS-208 MGS-LT

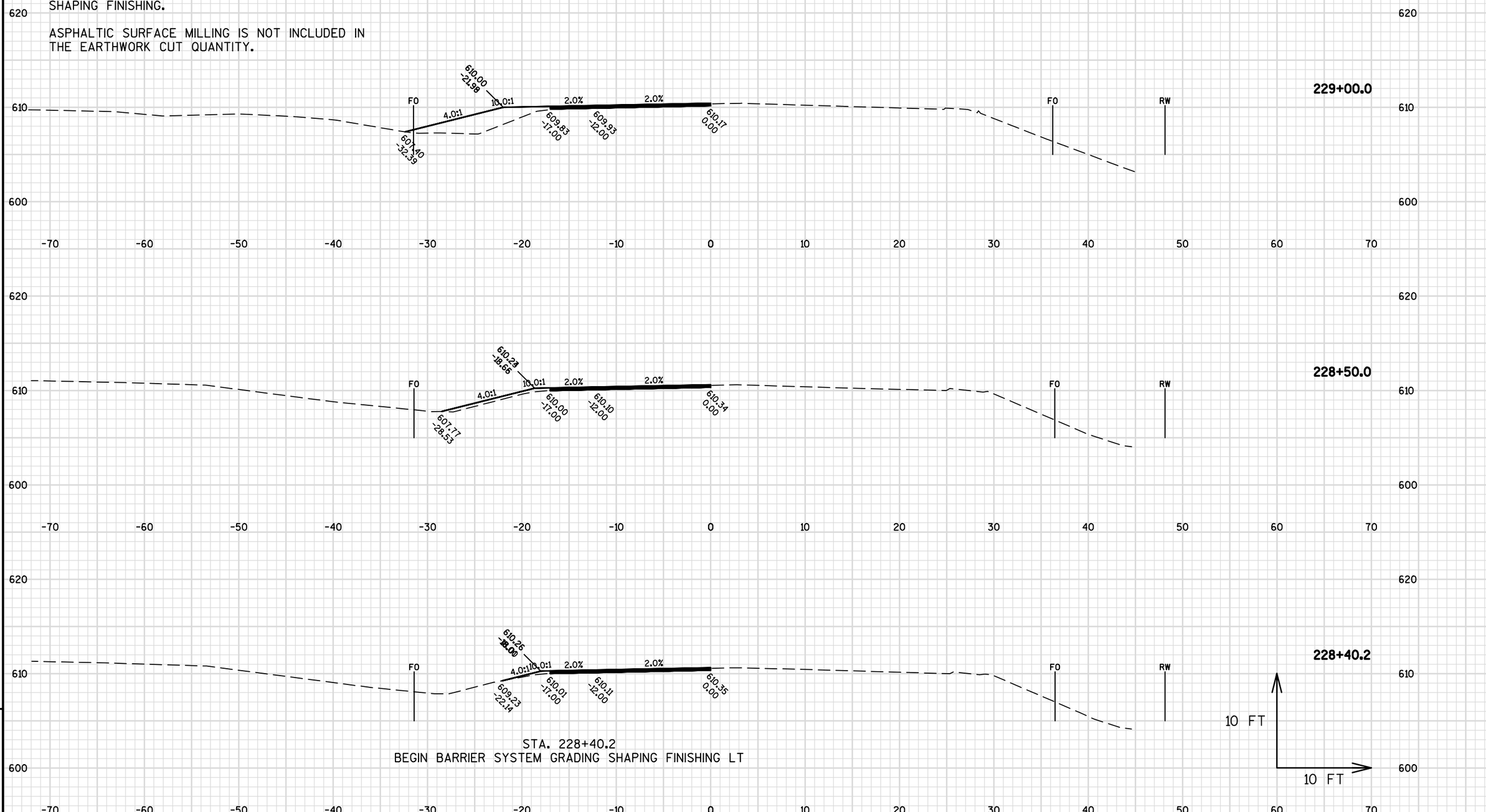
WISDOT/CADDS SHEET 49

NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.



STA. 228+40.2
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING LT

9

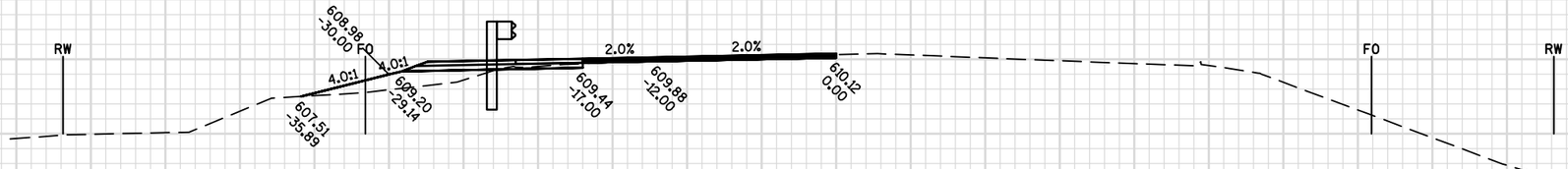
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

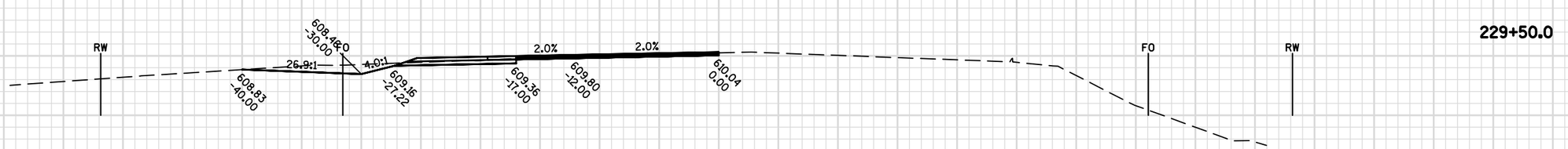
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG DATE : 2/12/2016 9:43 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 09020_(1)

WISDOT/CADDS SHEET 49

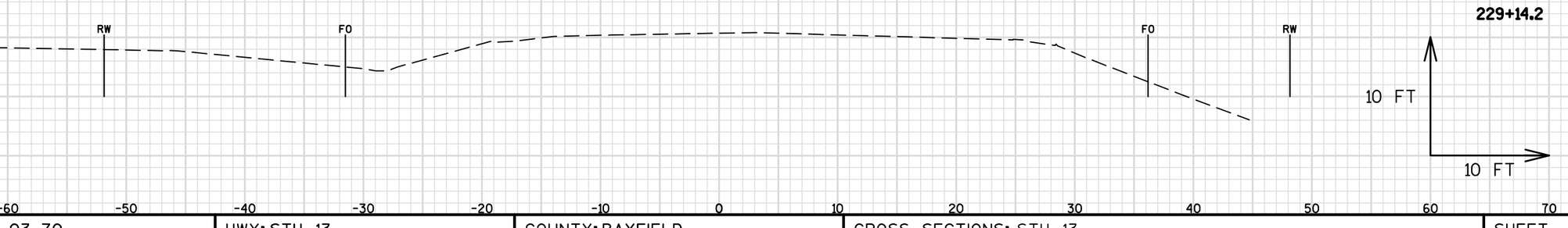
BEGIN EAT LT (POST NO. 1)



229+81.6



229+50.0



229+14.2

9

9

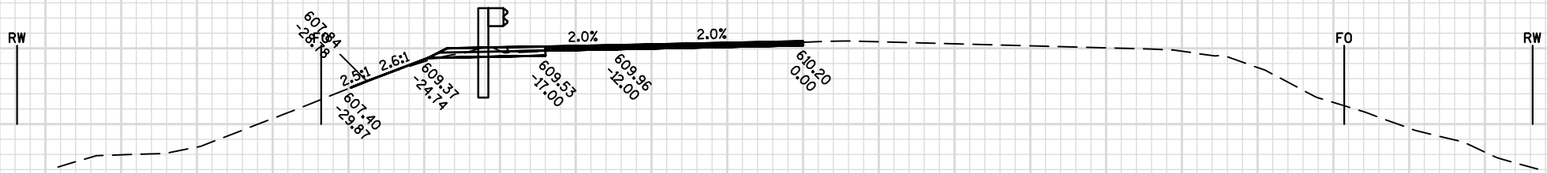
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 168	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(2)

PLOTTED DATE : 2/12/2016 9:44 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

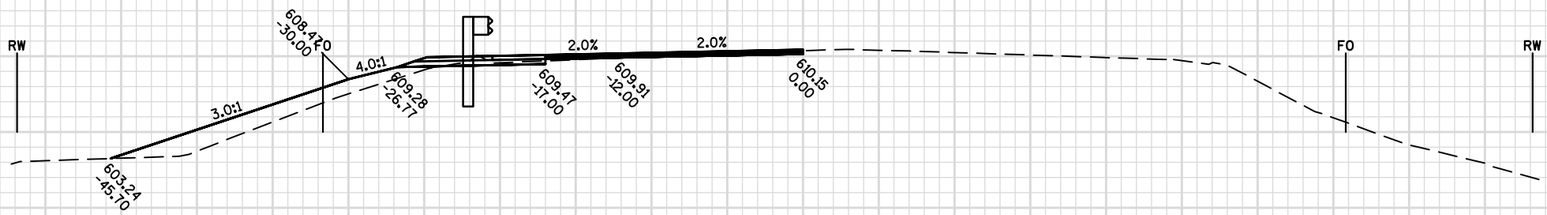
WISDOT/CADDS SHEET 49

END EAT LT (POST NO. 9)

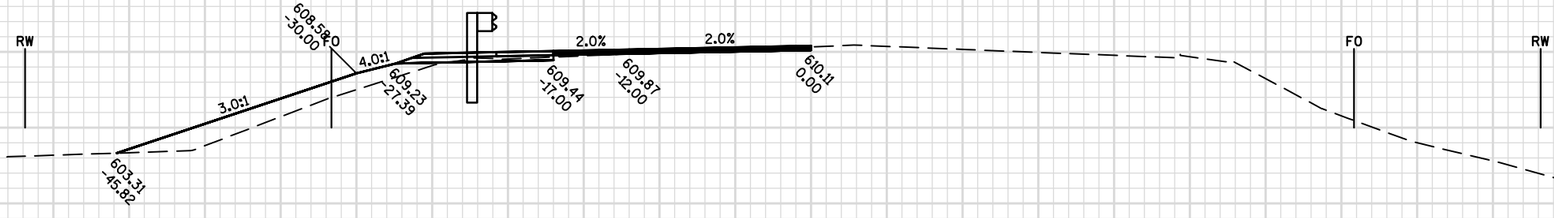


230+31.6

EAT LT (POST NO. 5)



230+06.6



230+00.0

10 FT
10 FT

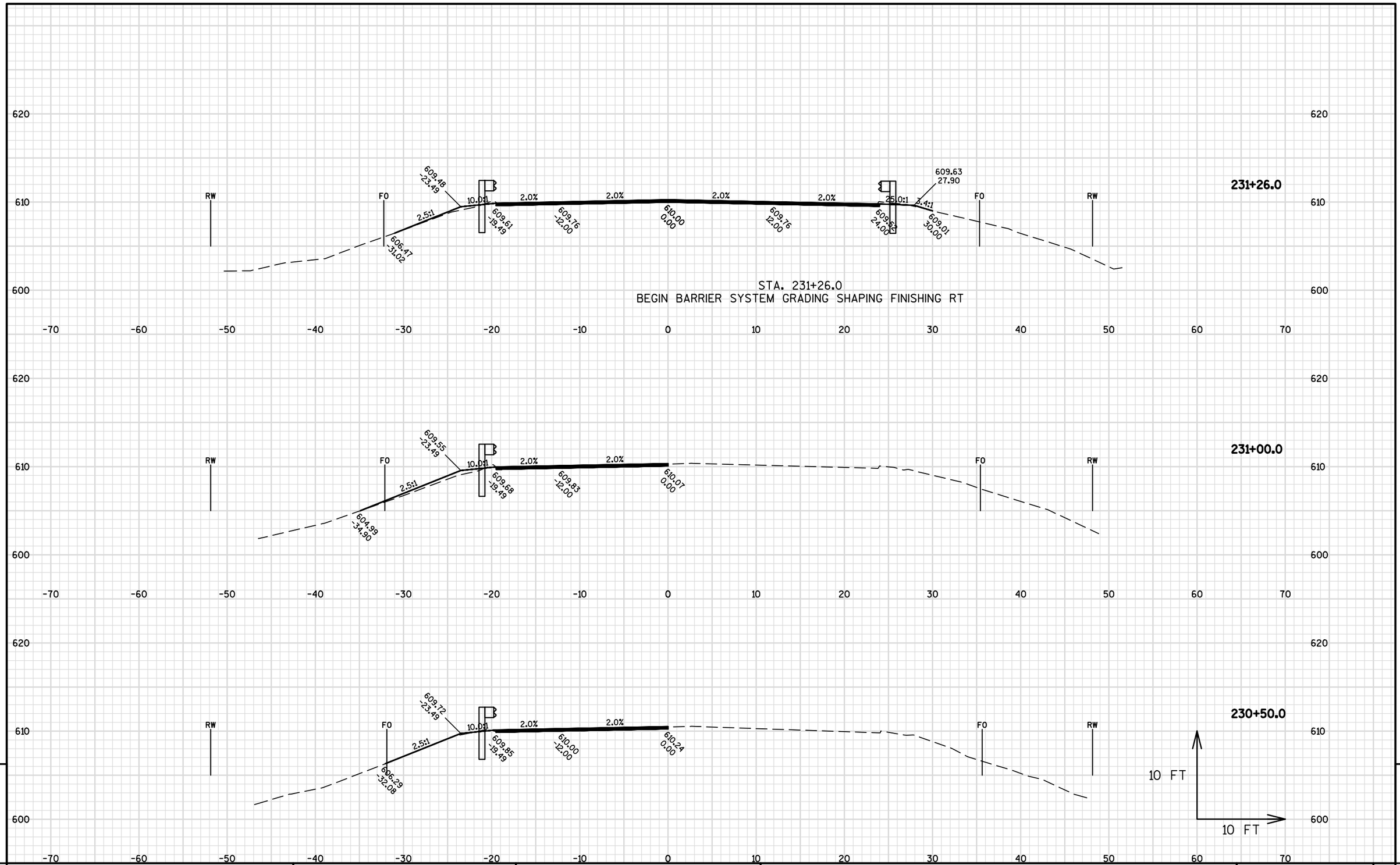
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 169	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(3)
PLOTTED DATE : 2/12/2016 9:44 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

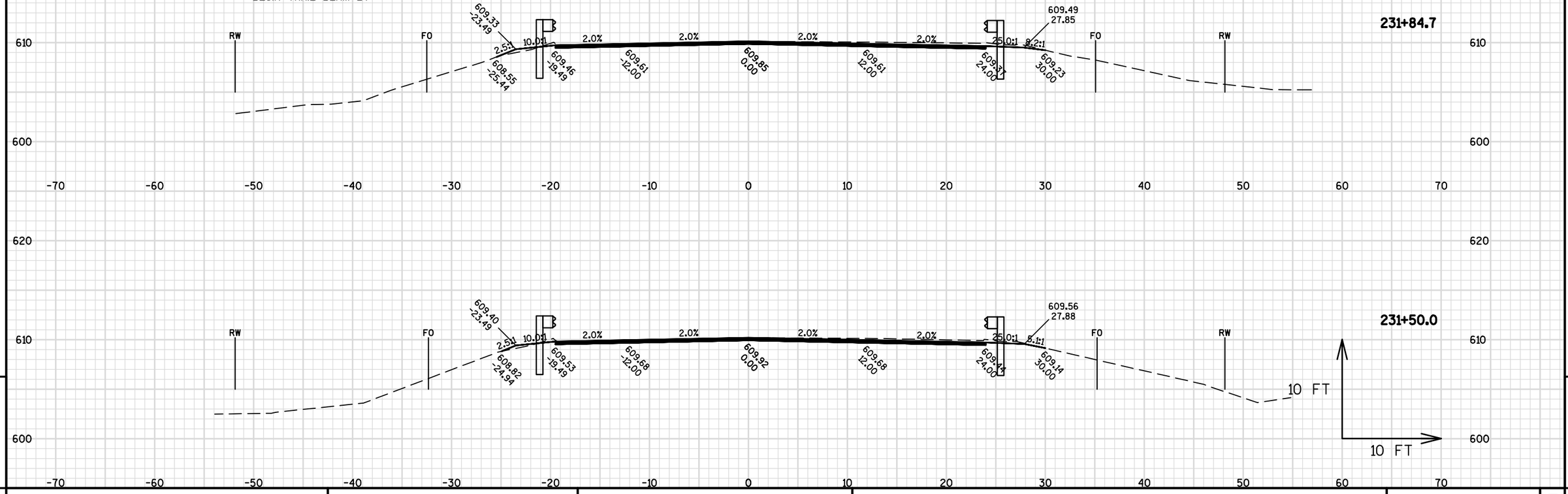


PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 170 E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(4)
PLOTTED DATE : 2/12/2016 9:44 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49

BEGIN THRIE BEAM RT

BEGIN THRIE BEAM LT

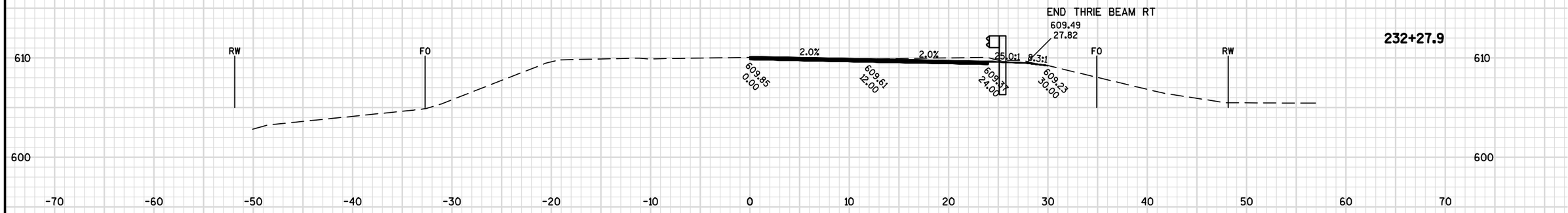


9

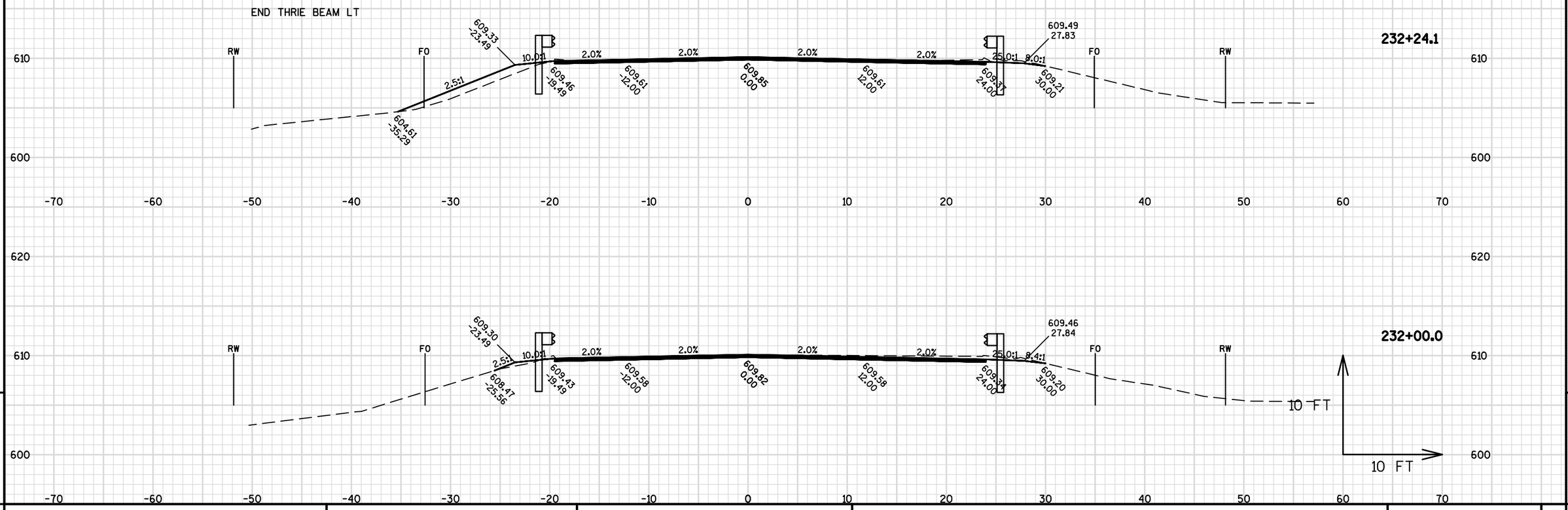
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 171	E

STA. 232+27.9
END BARRIER SYSTEM GRADING SHAPING FINISHING RT

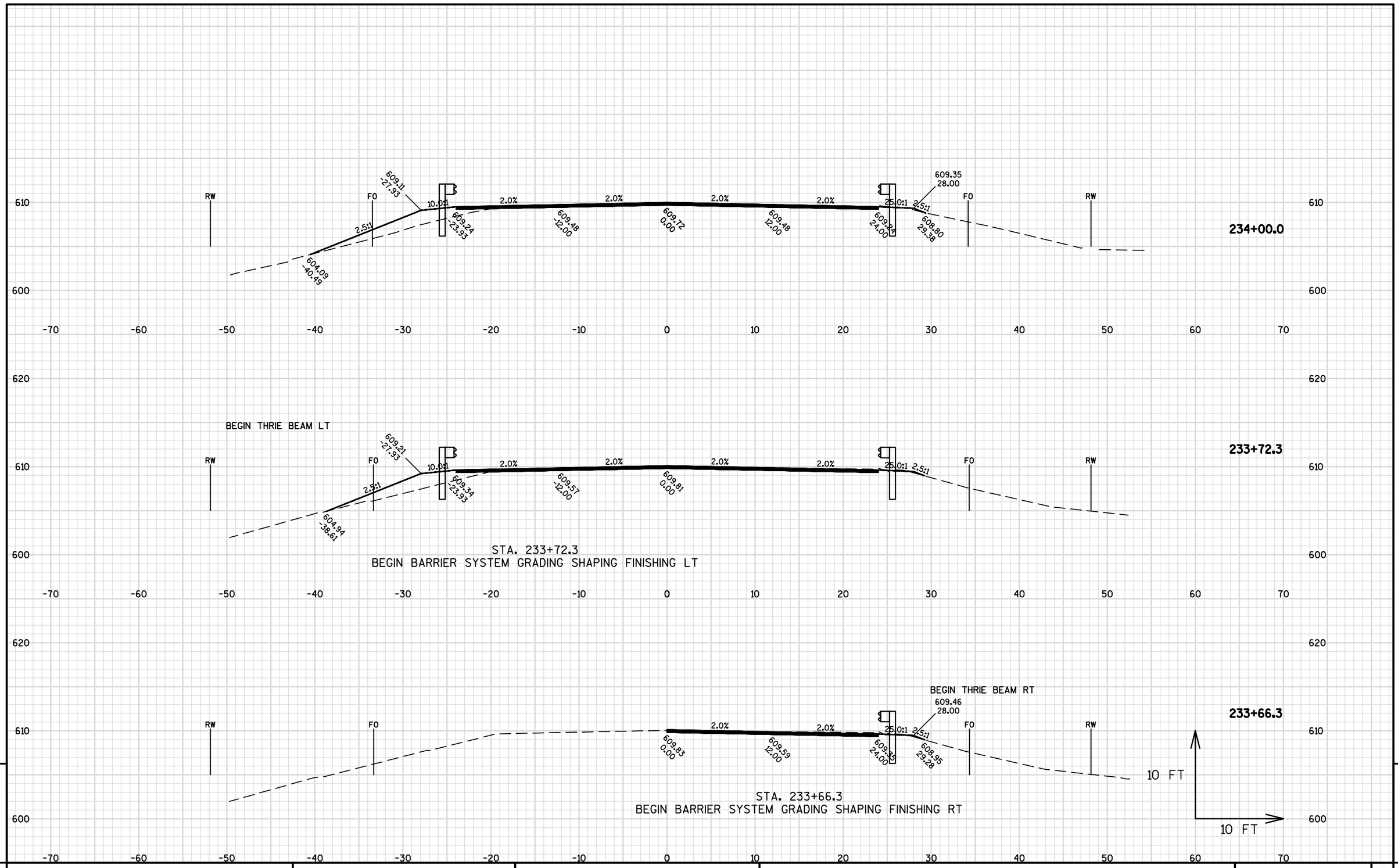


STA. 232+24.1
END BARRIER SYSTEM GRADING SHAPING FINISHING LT



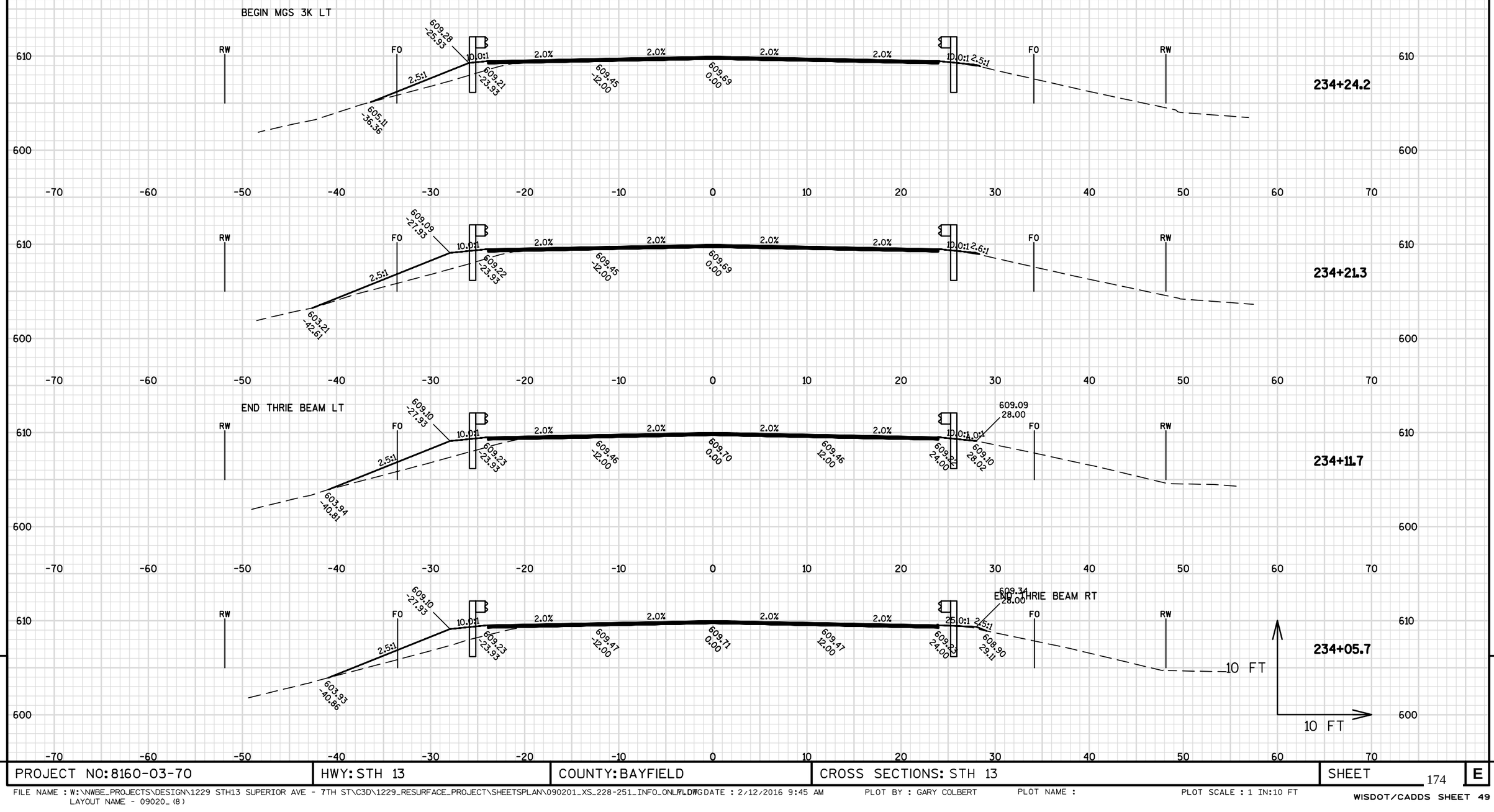
9 9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 172	E



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 173	E

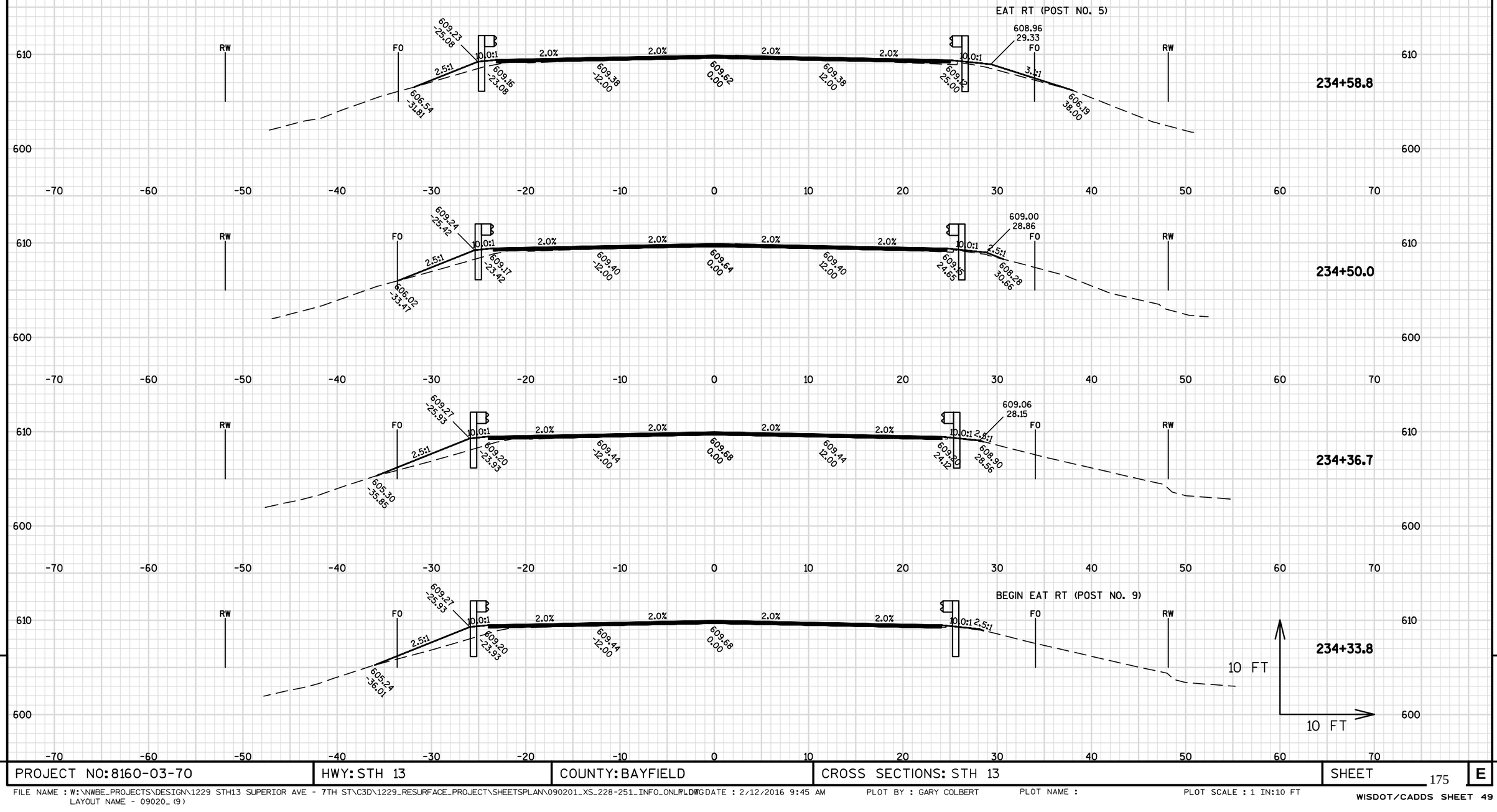
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(7)
PLOTTED DATE : 2/12/2016 9:45 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 174	E



9

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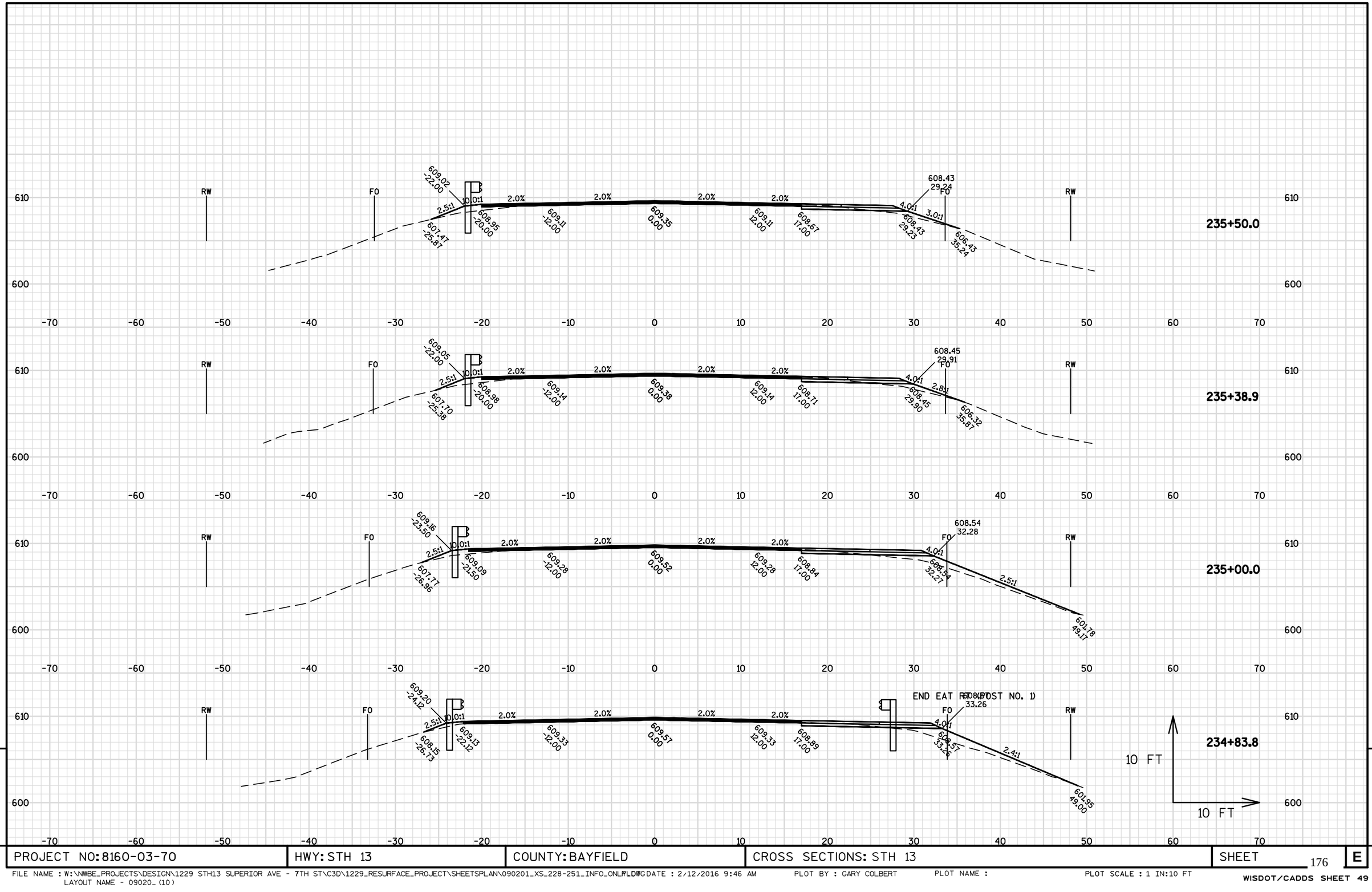
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(9)
DATE : 2/12/2016 9:45 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



9

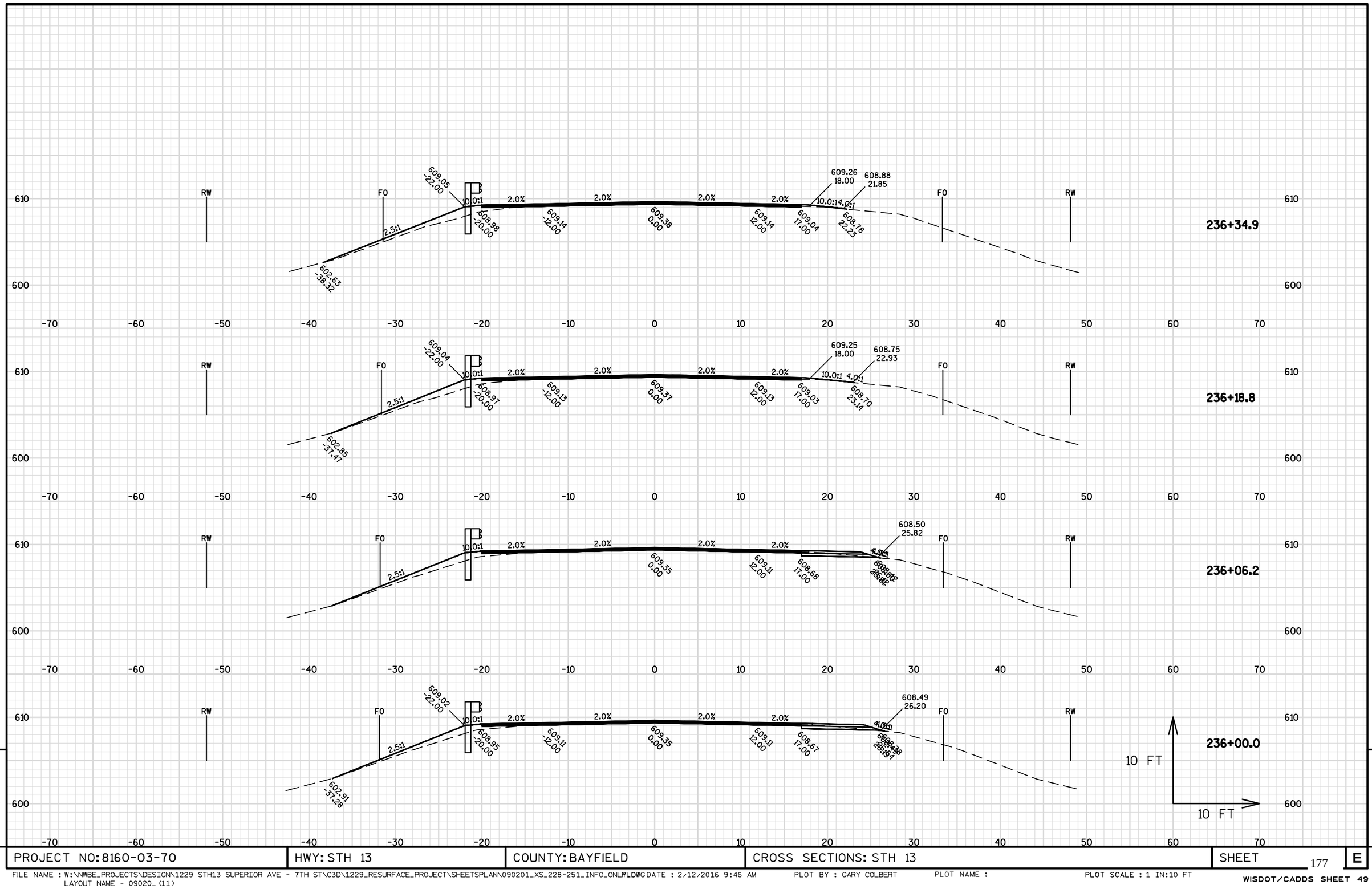
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(10)

PLOTTING DATE : 2/12/2016 9:46 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



9

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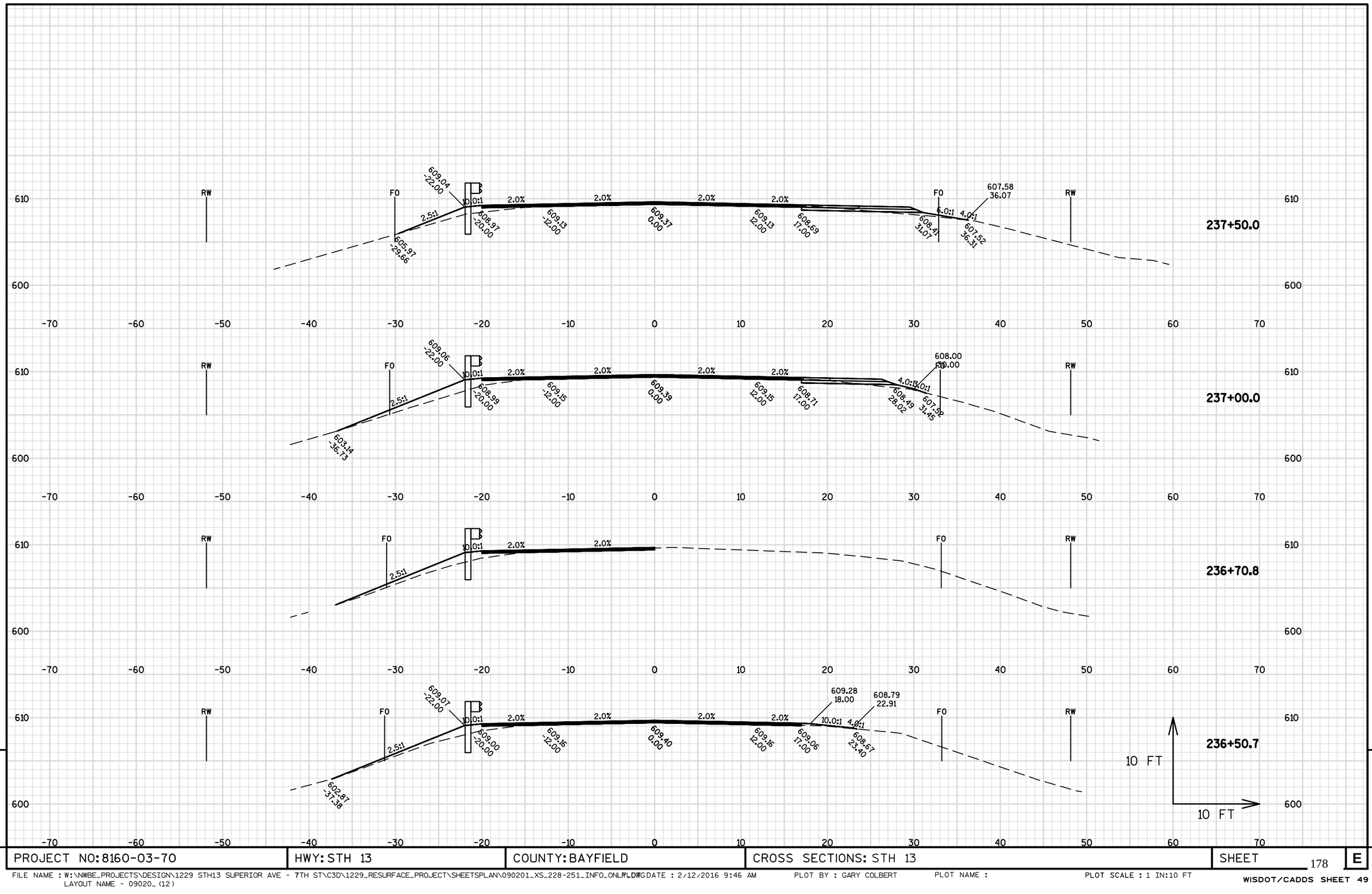
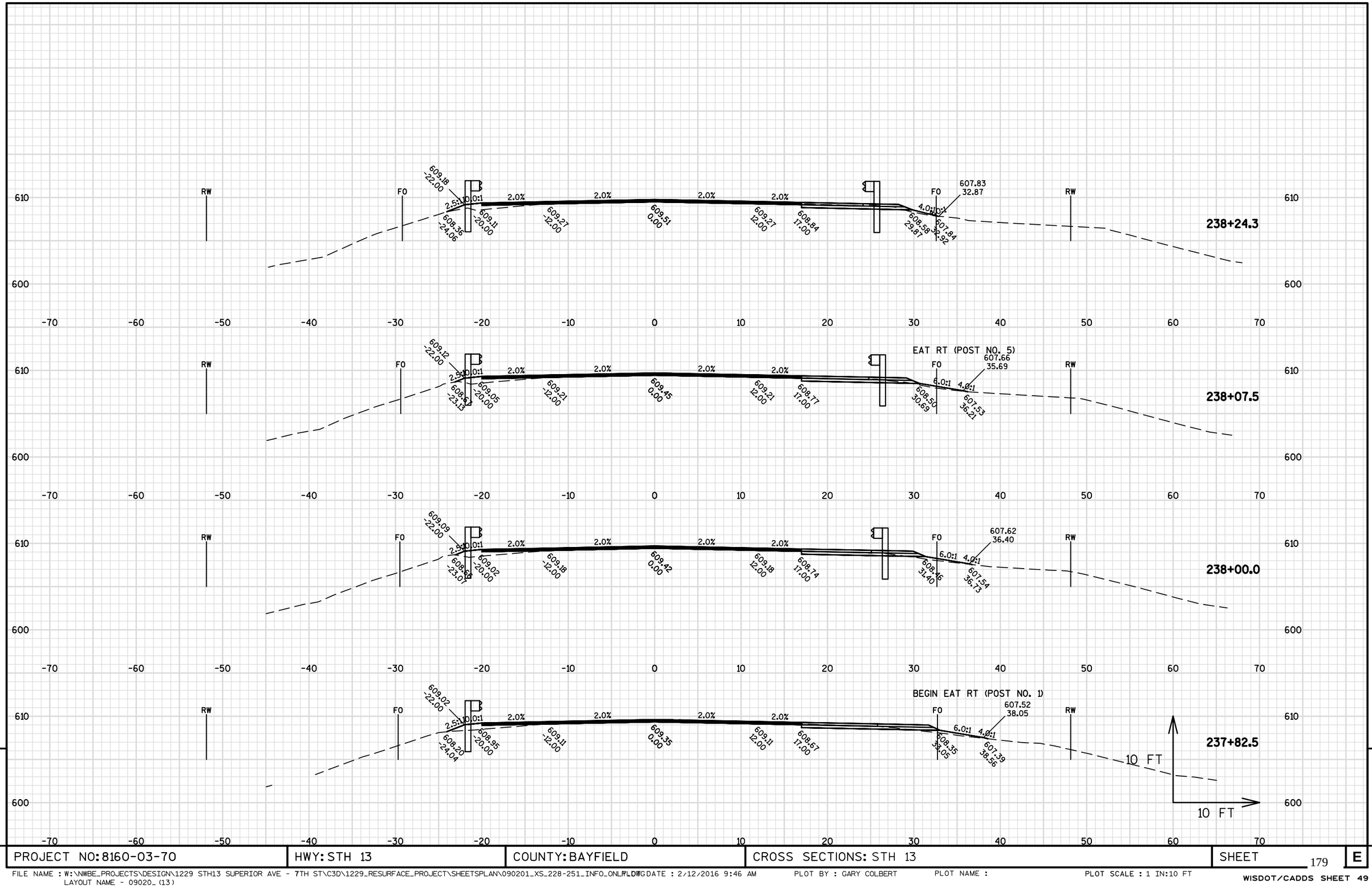
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 177	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(11)
PLOTTED DATE : 2/12/2016 9:46 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

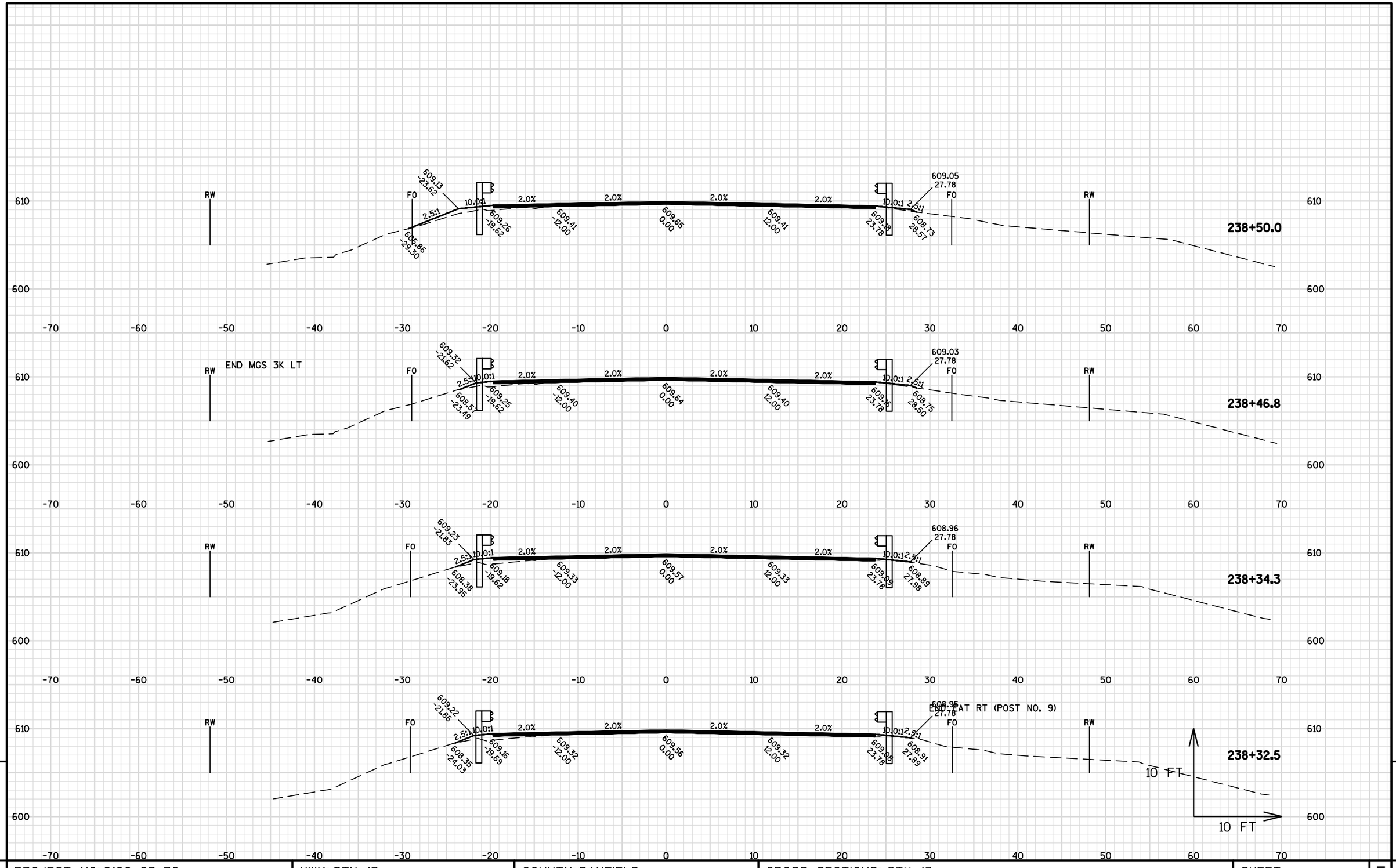


PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 178	E



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG LAYOUT NAME - 09020_(13)
PLOTTED DATE : 2/12/2016 9:46 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49

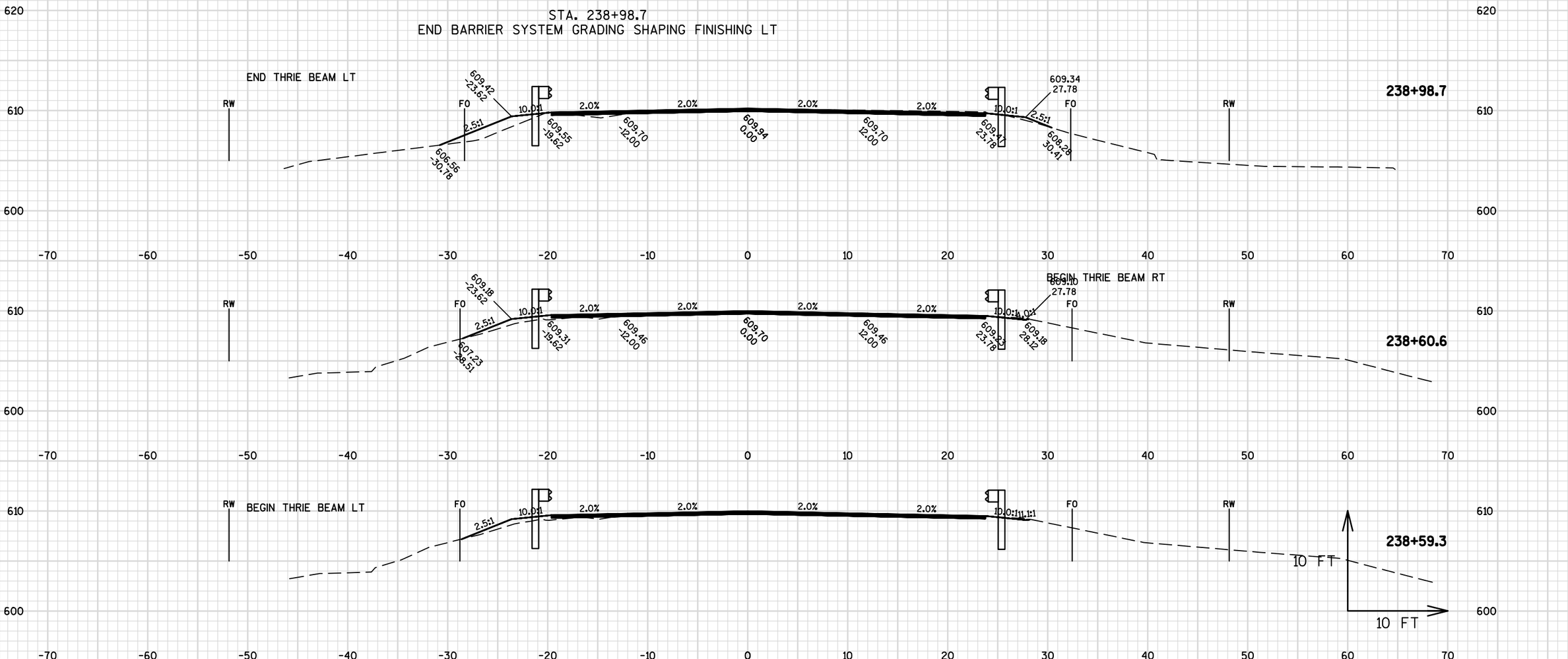


9

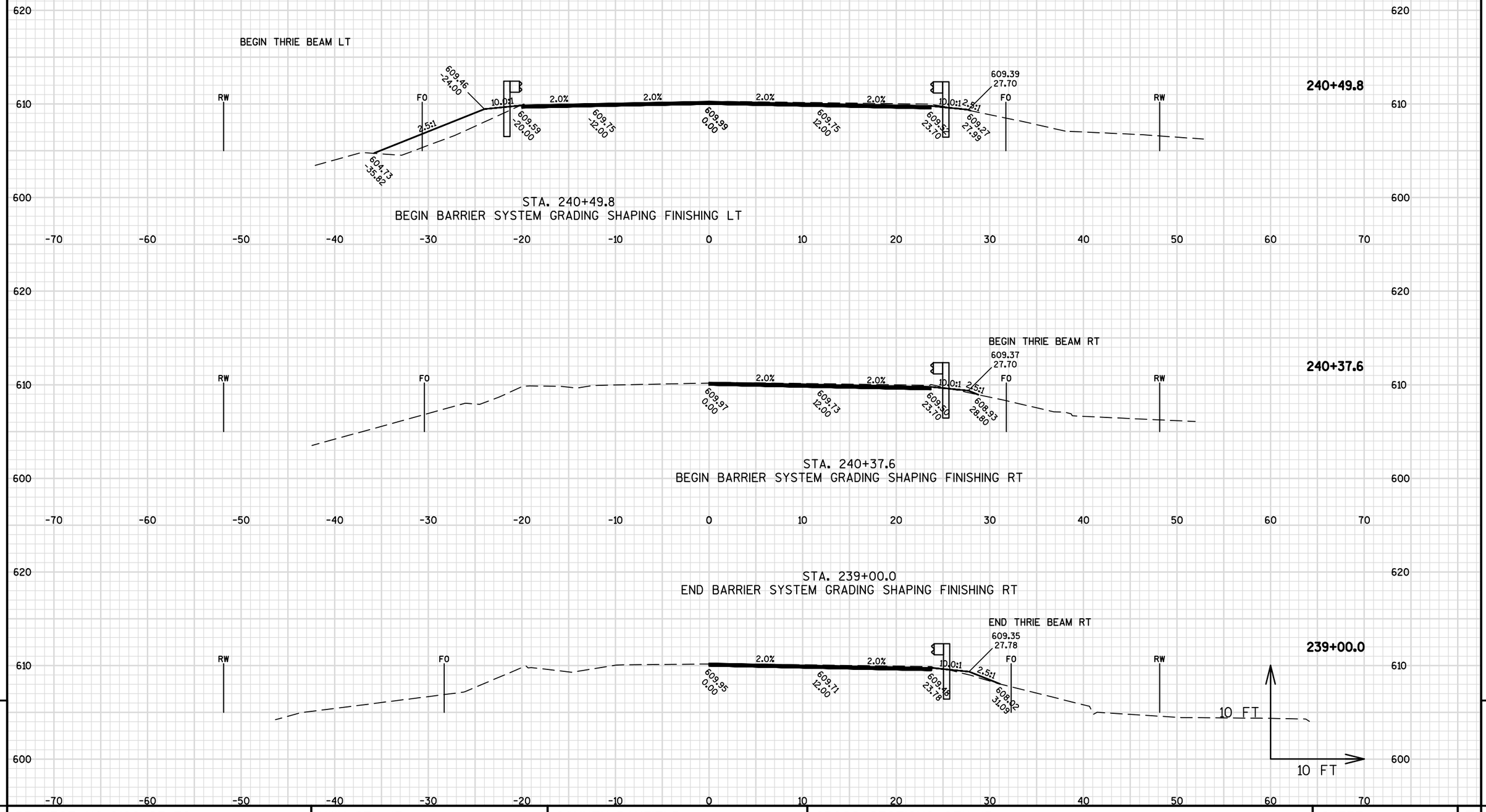
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 180	E

STA. 238+98.7
END BARRIER SYSTEM GRADING SHAPING FINISHING LT



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 181 E



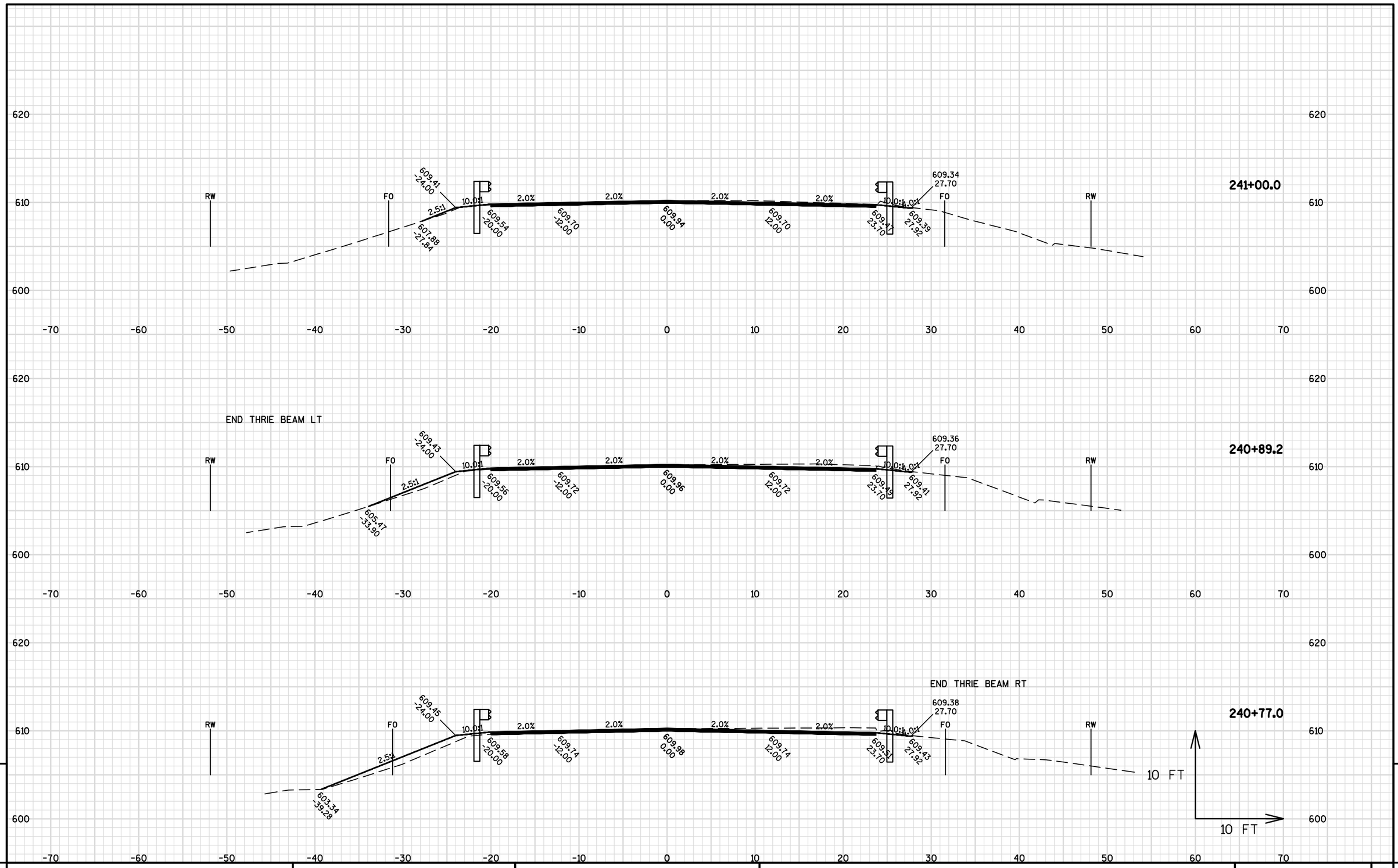
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(16)
PLOTTED DATE : 2/12/2016 9:47 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



9

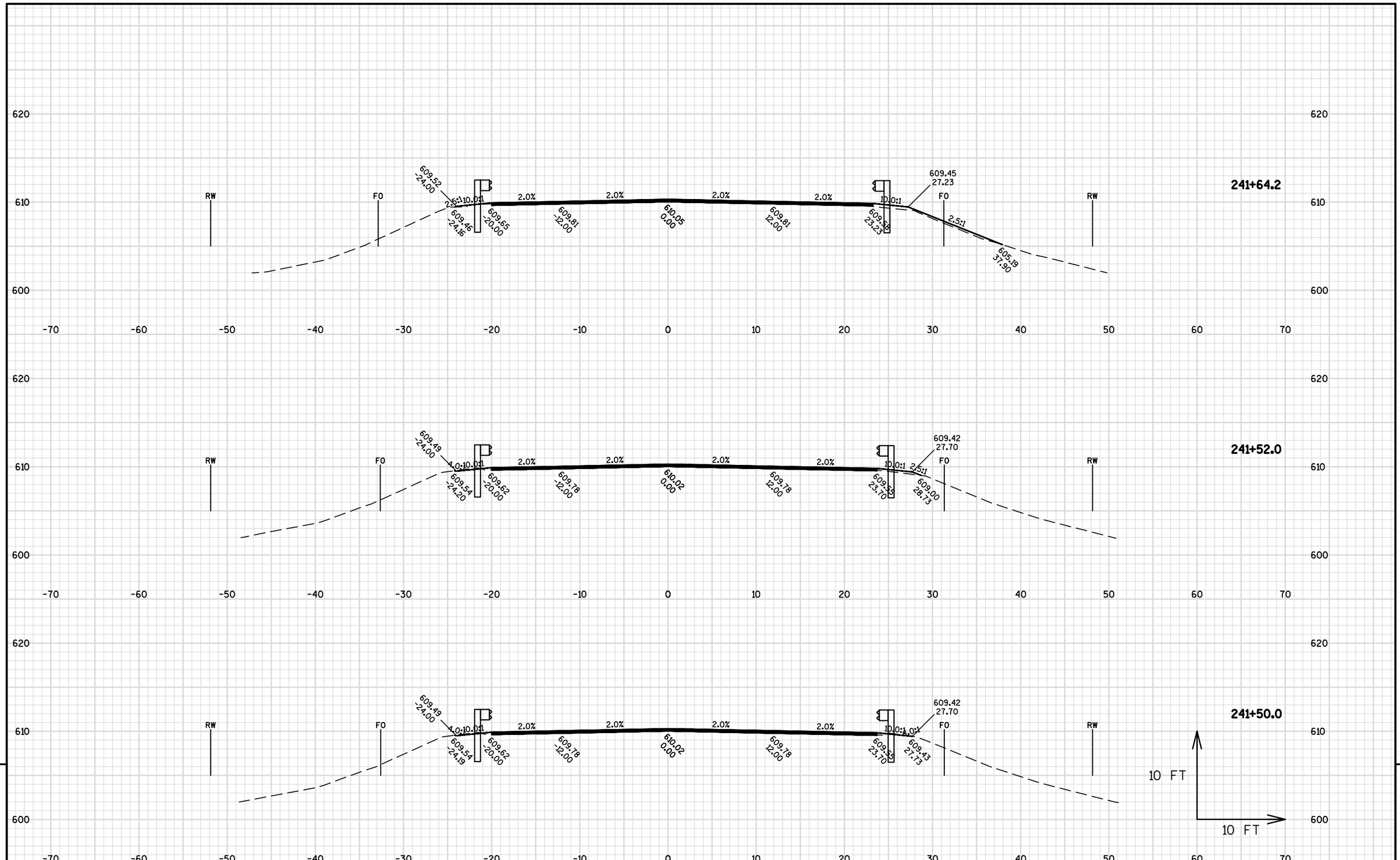
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 183	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(17)

DATE : 2/12/2016 9:48 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



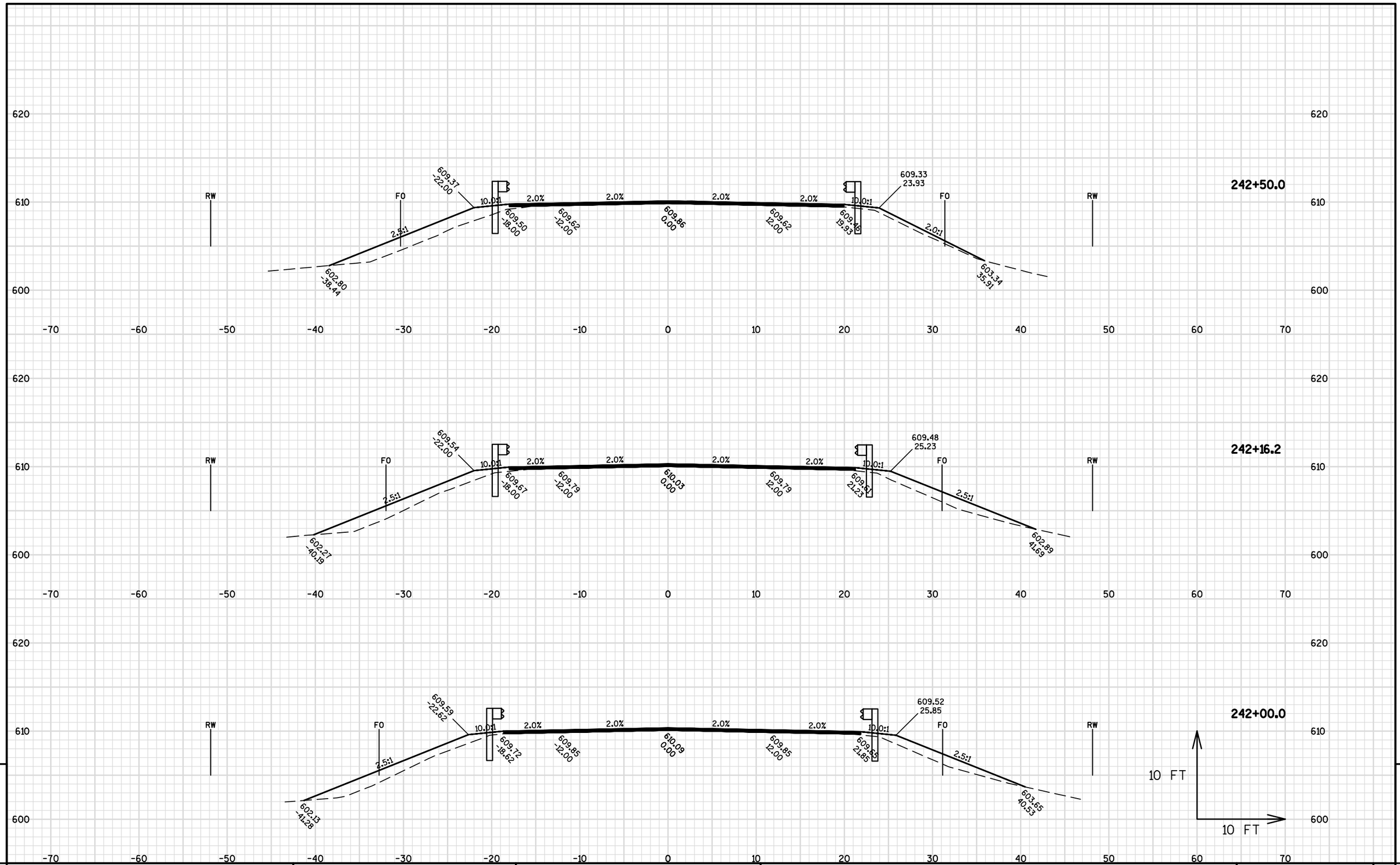
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 184	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(18)
PLOTTED : 2/12/2016 9:48 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



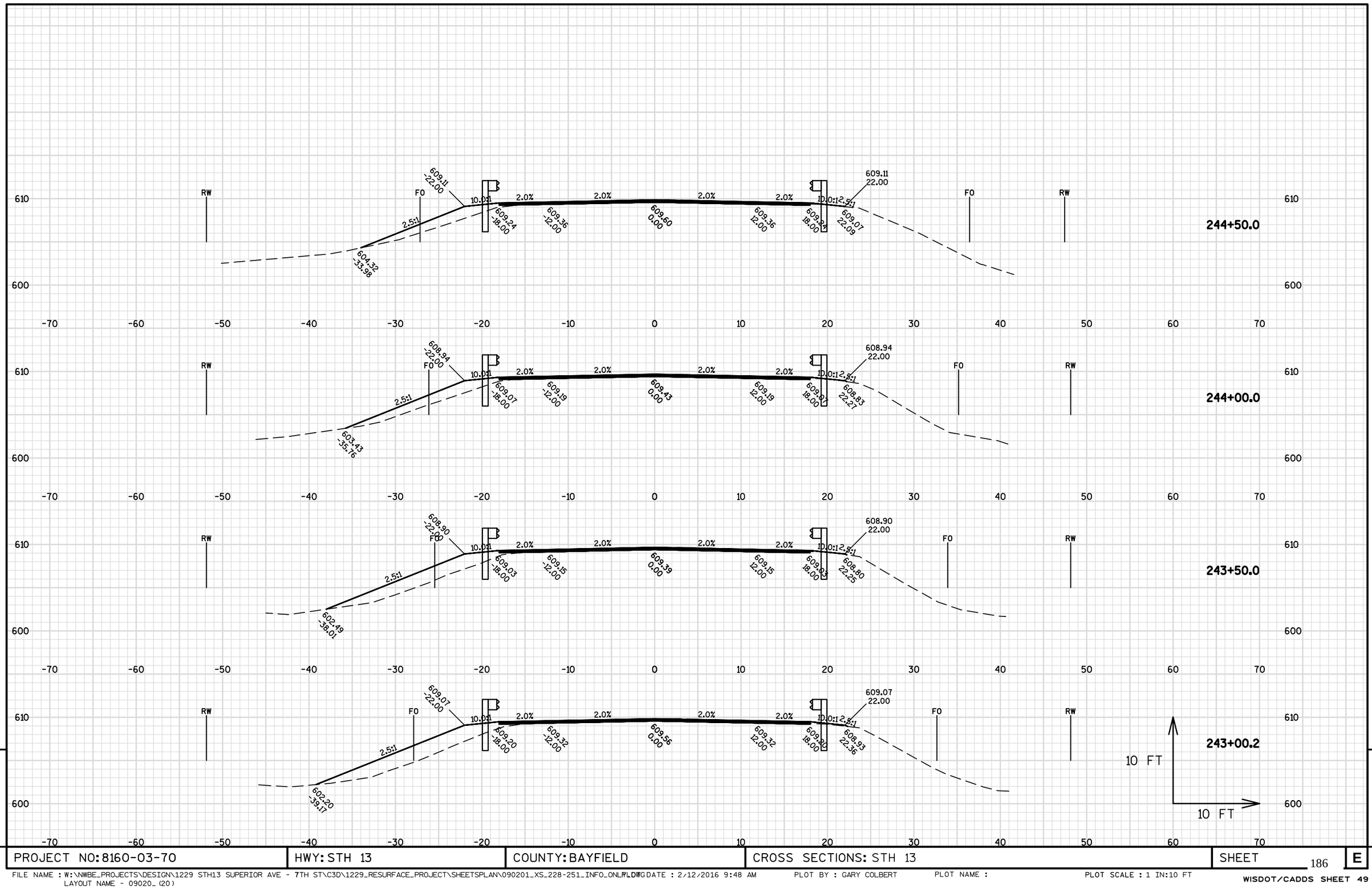
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 185	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(19)
PLOTTED : 2/12/2016 9:48 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

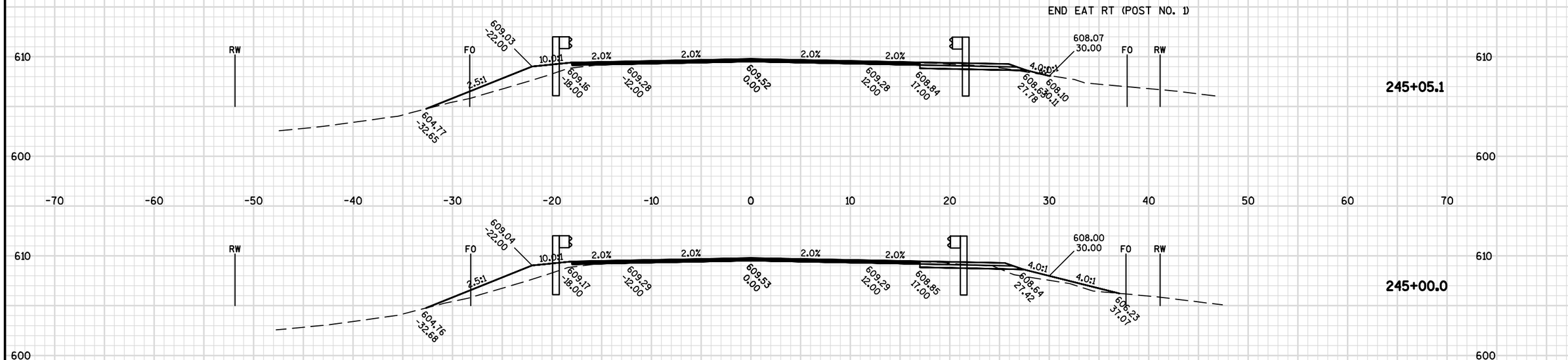


9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(20)
PLOTTED DATE : 2/12/2016 9:48 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

END EAT RT (POST NO. 1)

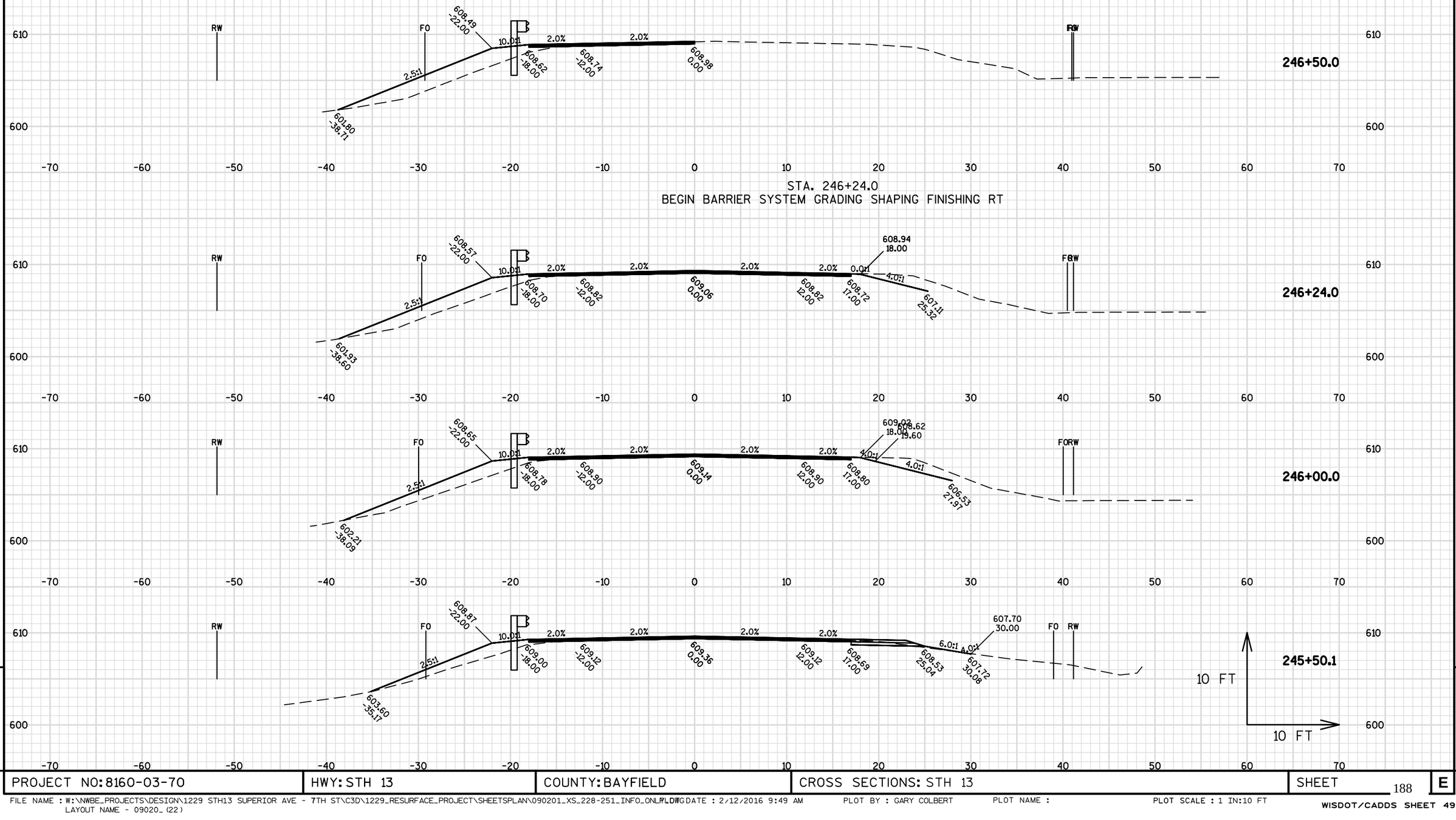


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(21)
PLOTTING DATE : 2/12/2016 9:49 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

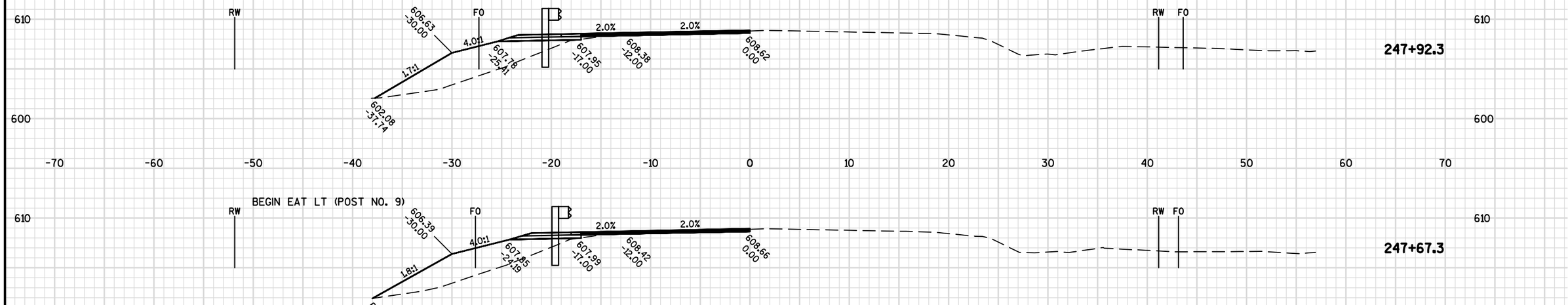


9

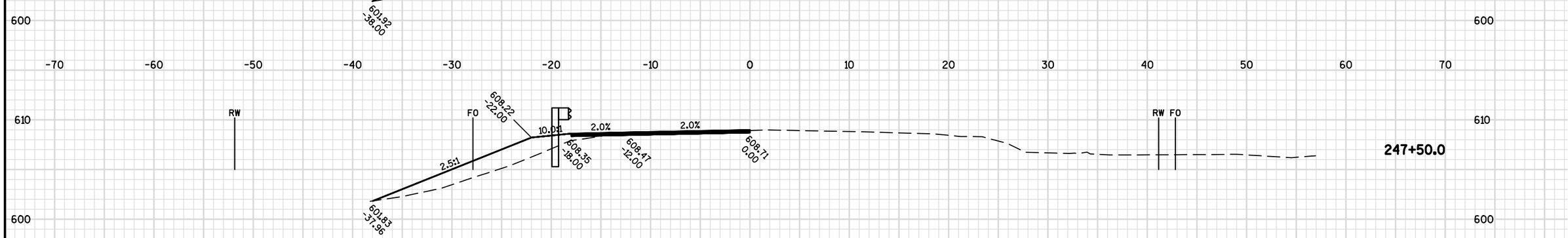
9

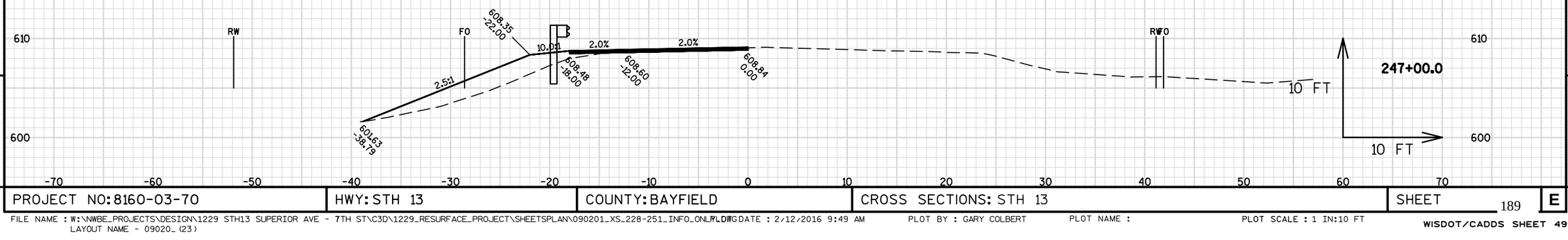
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 188	E

EAT LT (POST NO. 5)



BEGIN EAT LT (POST NO. 9)





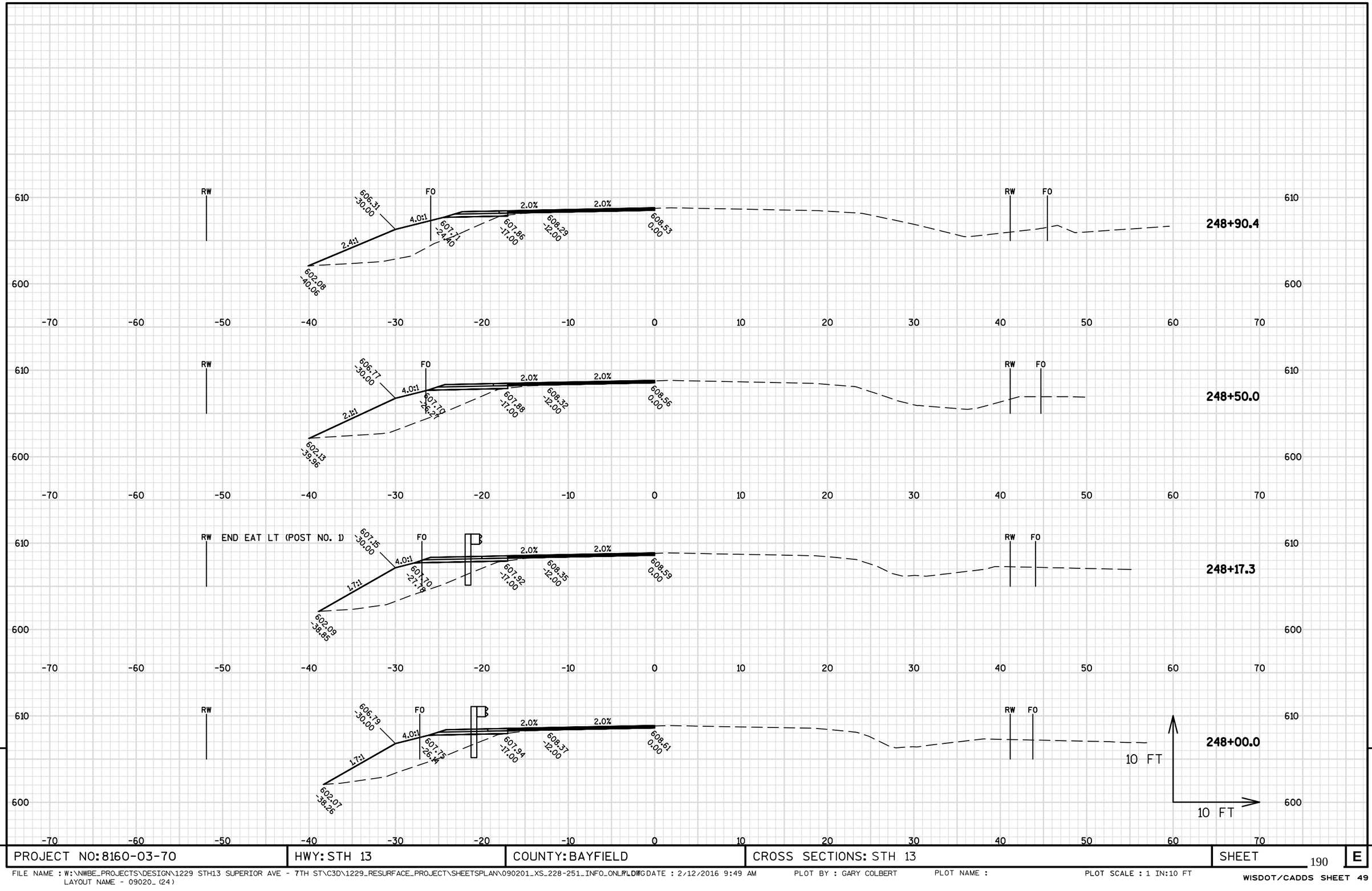
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(23)
PLOTTED : 2/12/2016 9:49 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



9

9

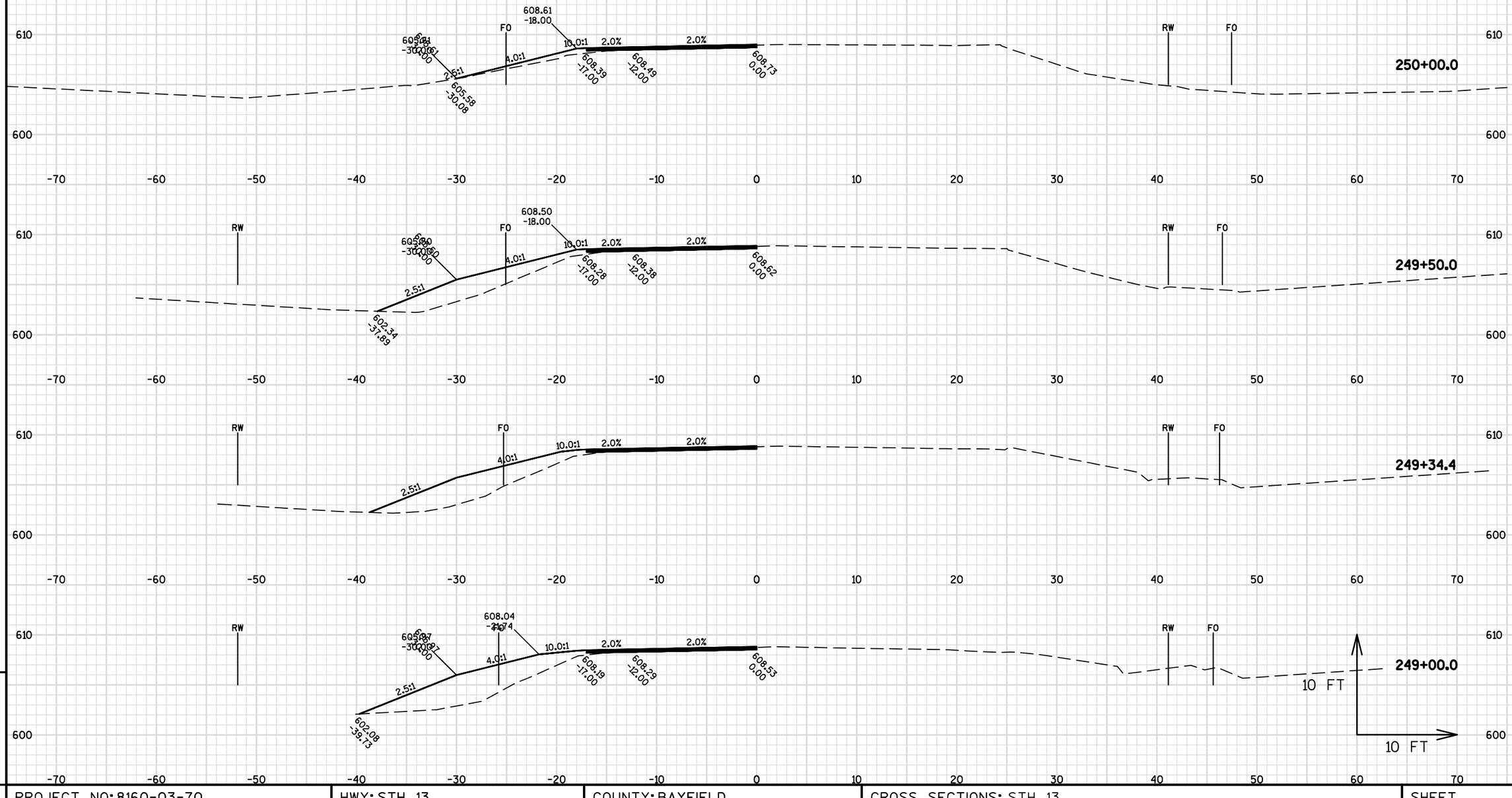
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(24)

PLOTTED : 2/12/2016 9:49 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

STA. 250+00.0
END BARRIER SYSTEM GRADING SHAPING FINISHING LT



9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 191	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090201_XS_228-251_INFO_ONLY.DWG
LAYOUT NAME - 09020_(25)
PLOTTED DATE : 2/12/2016 9:50 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

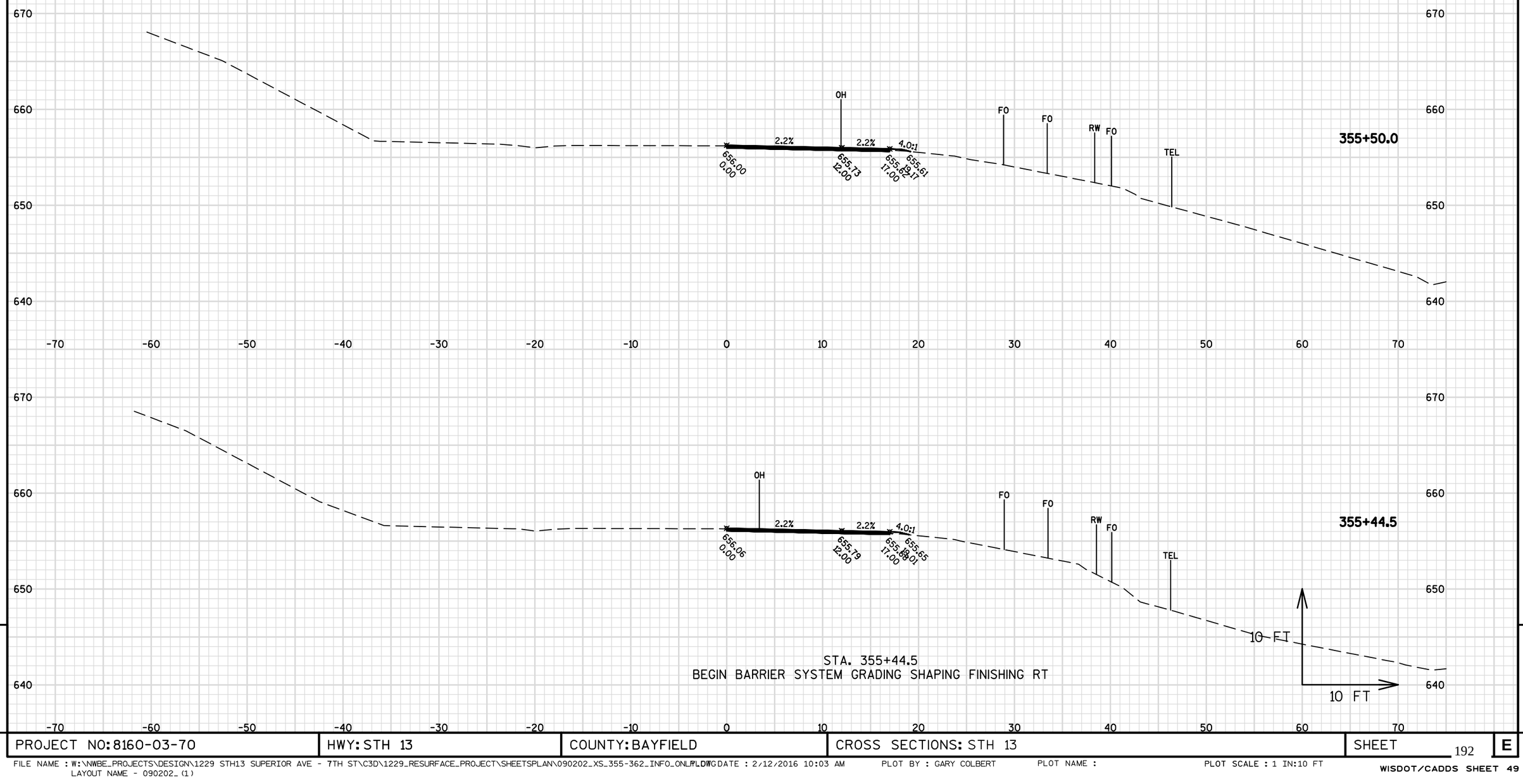
WISDOT/CADDS SHEET 49

NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.



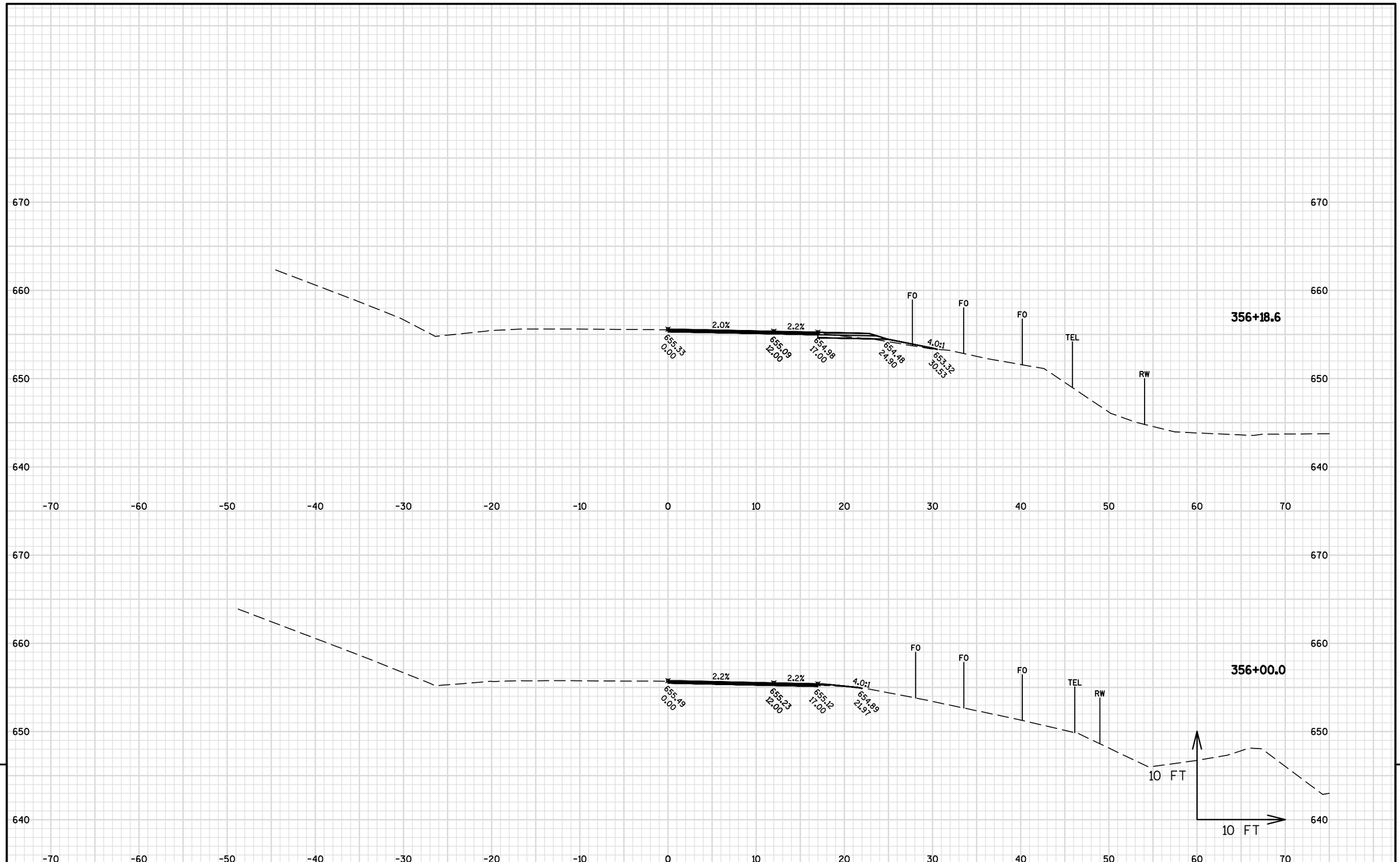
STA. 355+44.5
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING RT

9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG
LAYOUT NAME - 090202_(1)
PLOTTED DATE : 2/12/2016 10:03 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



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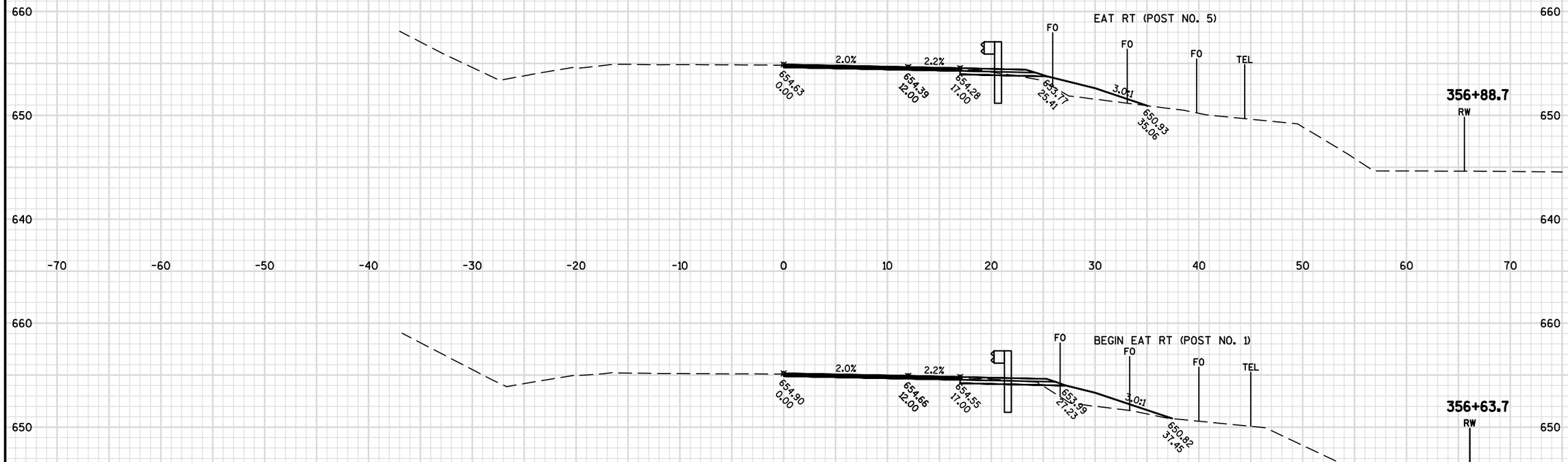
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 193 E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG DATE : 2/12/2016 10:03 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090202_(2)

WISDOT/CADDS SHEET 49

EAT RT (POST NO. 5)



BEGIN EAT RT (POST NO. 1)

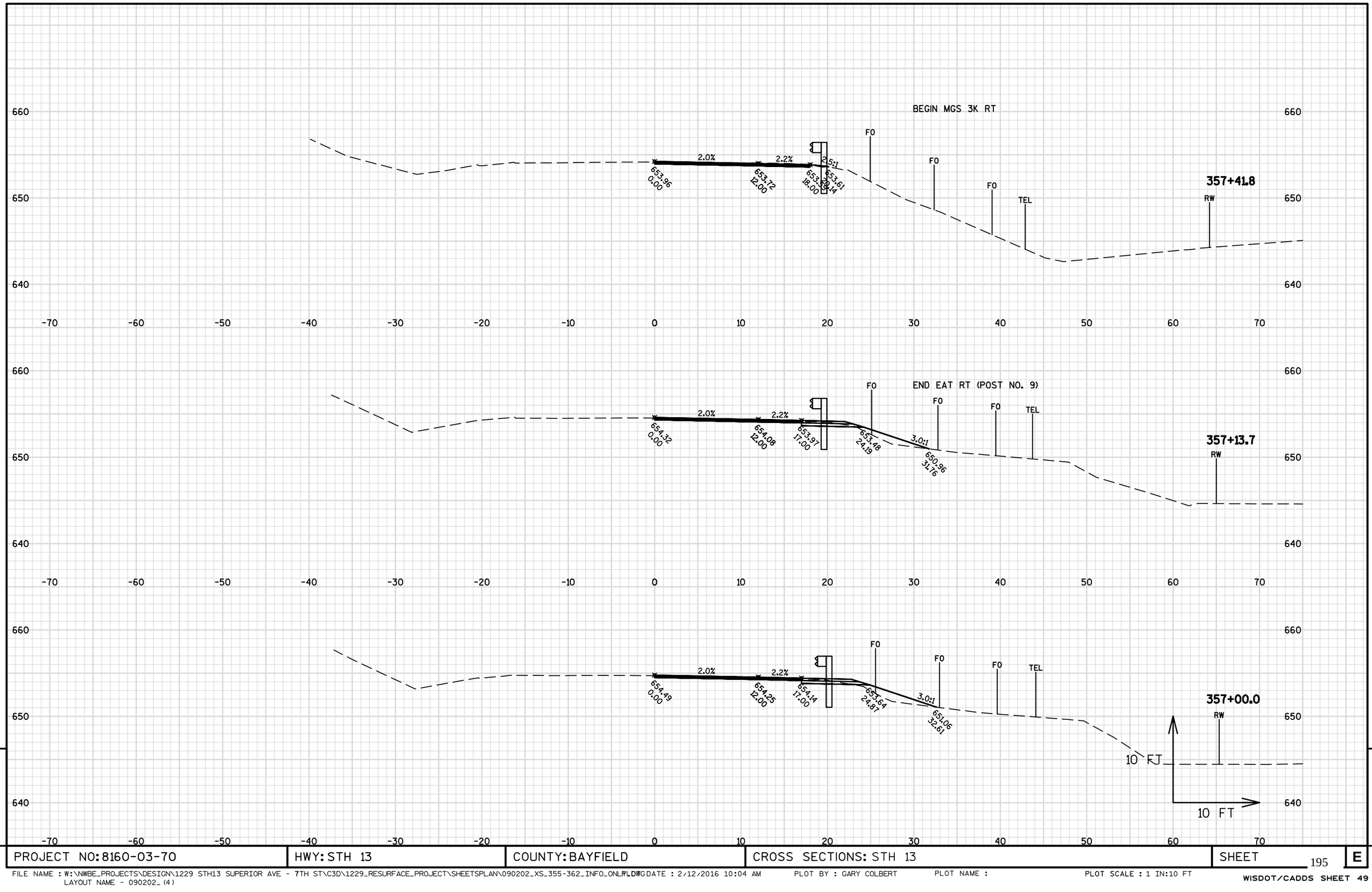
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 194	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG
LAYOUT NAME - 090202_(3)
PLOTTED DATE : 2/12/2016 10:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



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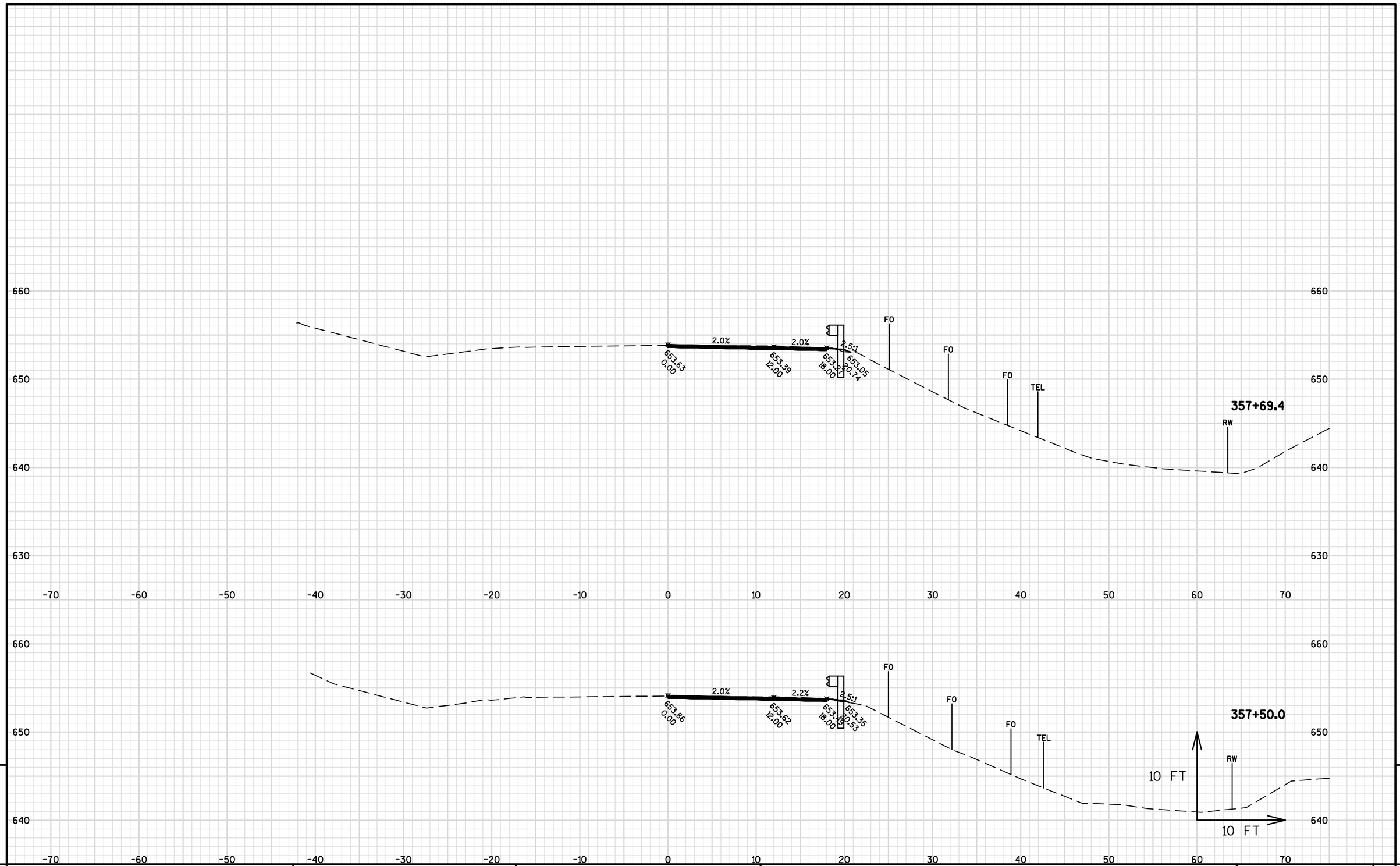
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG
LAYOUT NAME - 090202_(4)

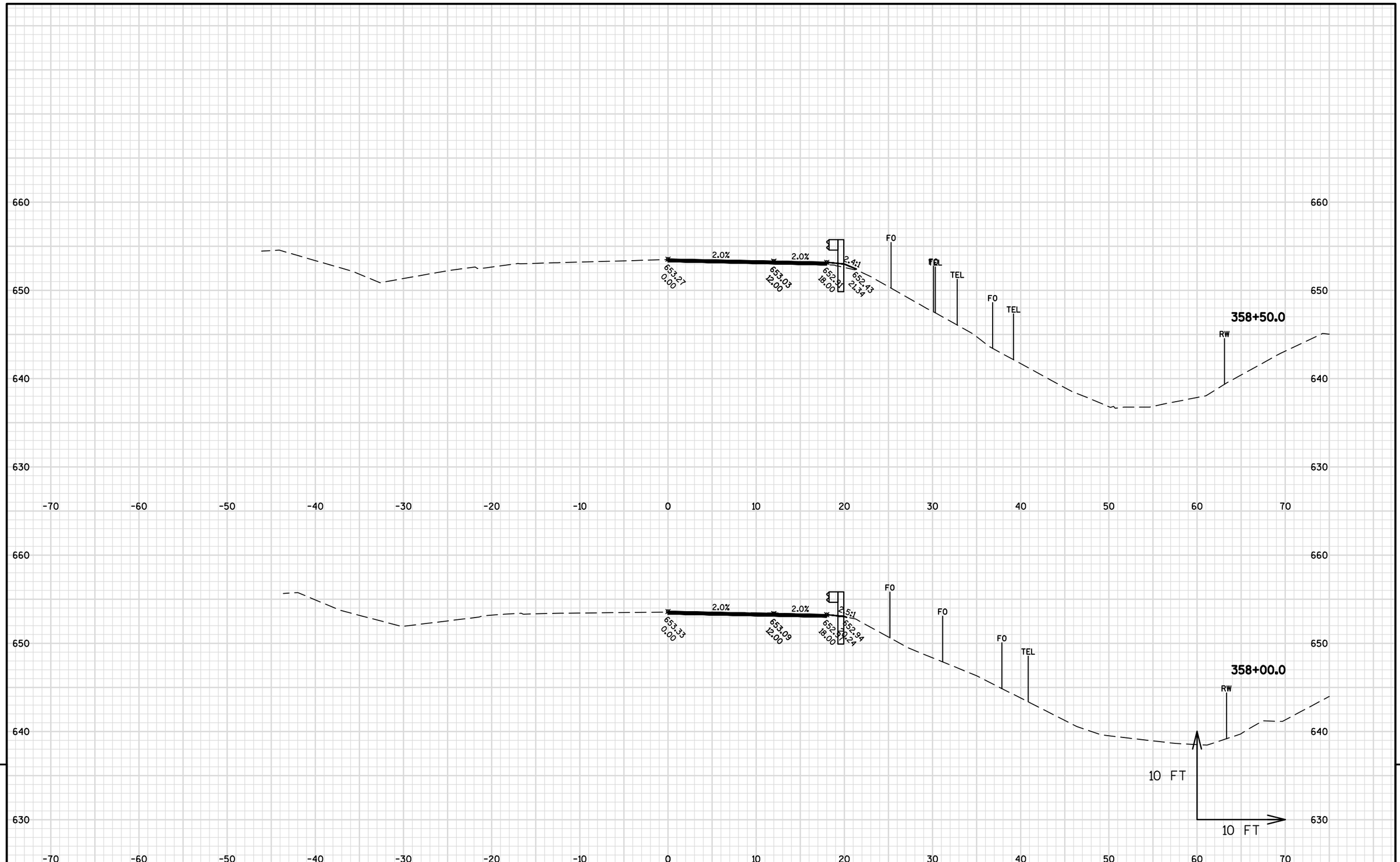
PLOT DATE : 2/12/2016 10:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG PLOTTED : 2/12/2016 10:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090202_(5)



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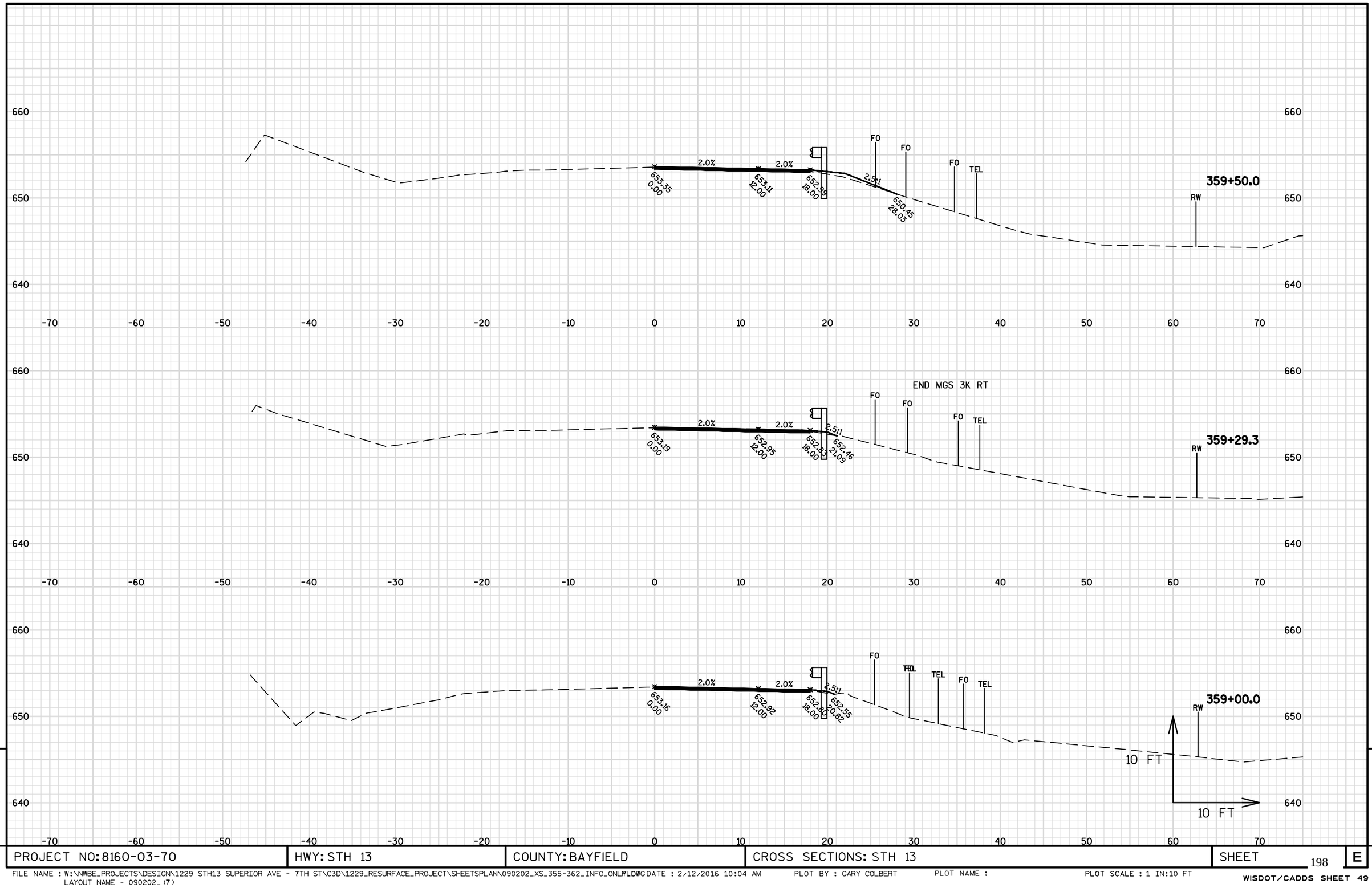
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 197	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG DATE : 2/12/2016 10:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090202_(6)

WISDOT/CADDS SHEET 49



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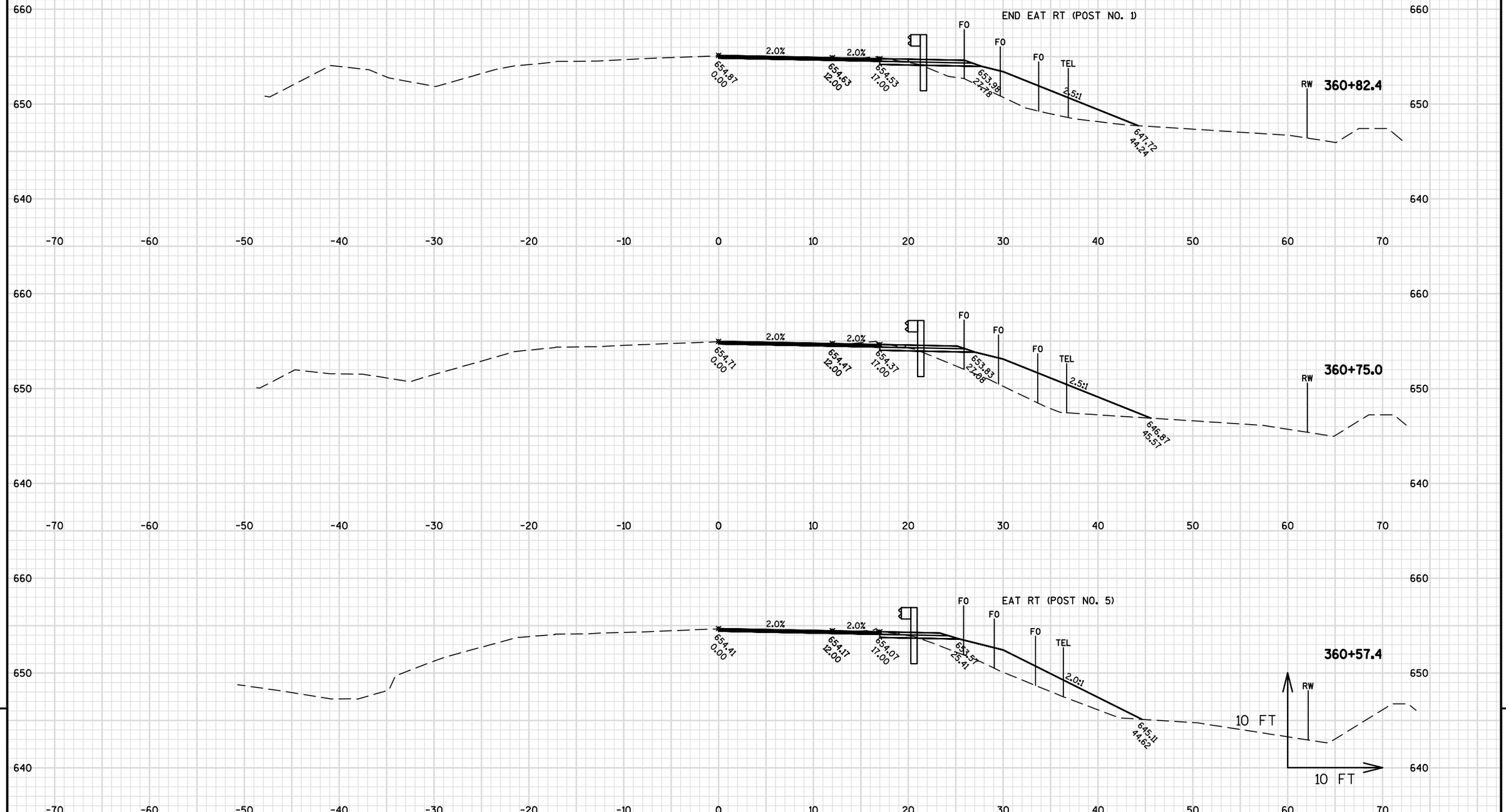
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG
LAYOUT NAME - 090202_(7)
PLOTTED : 2/12/2016 10:04 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG
LAYOUT NAME - 090202_(8)
PLOTTED DATE : 2/12/2016 10:05 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

END EAT RT (POST NO. 1)



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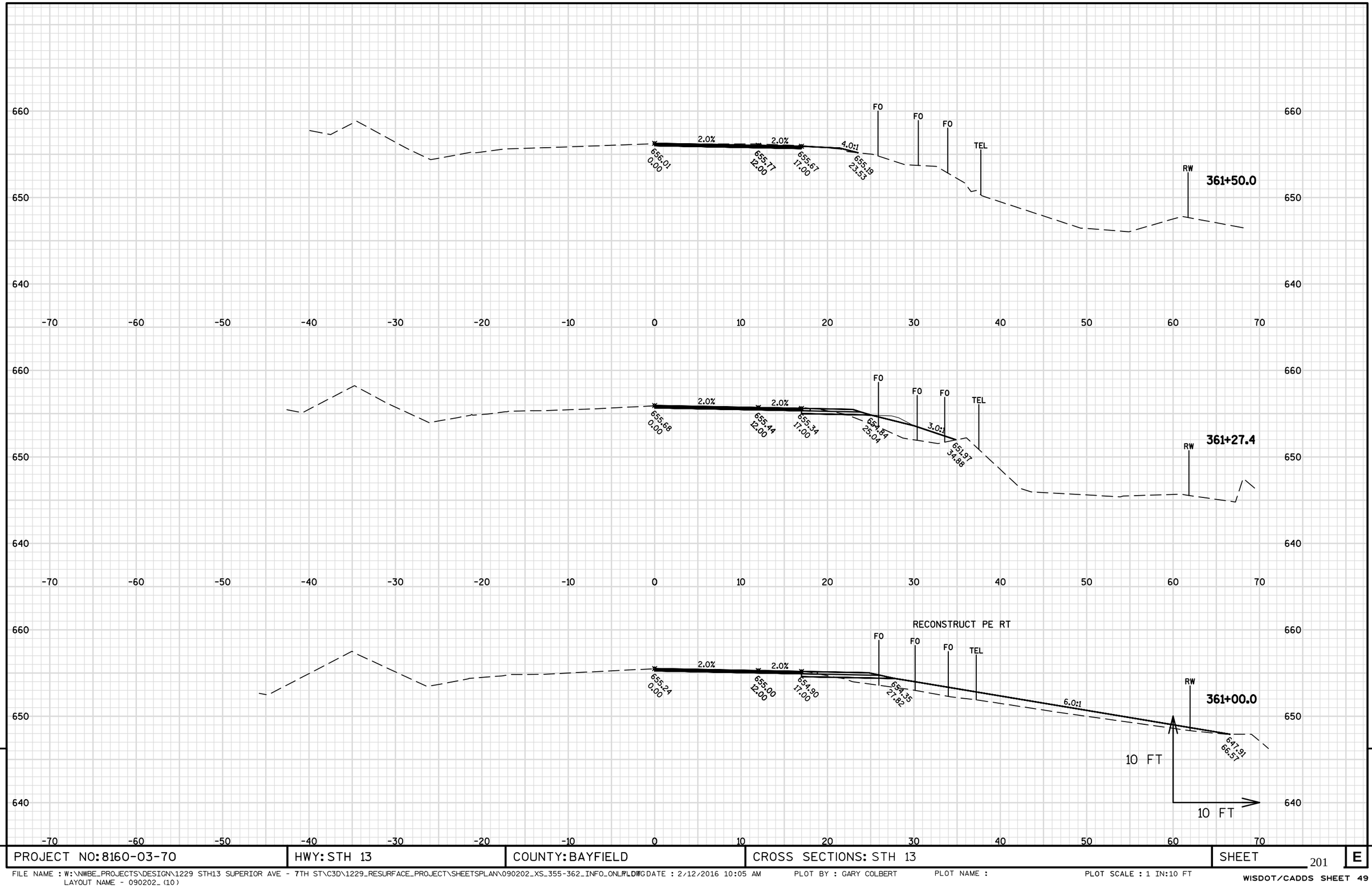
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 200	E

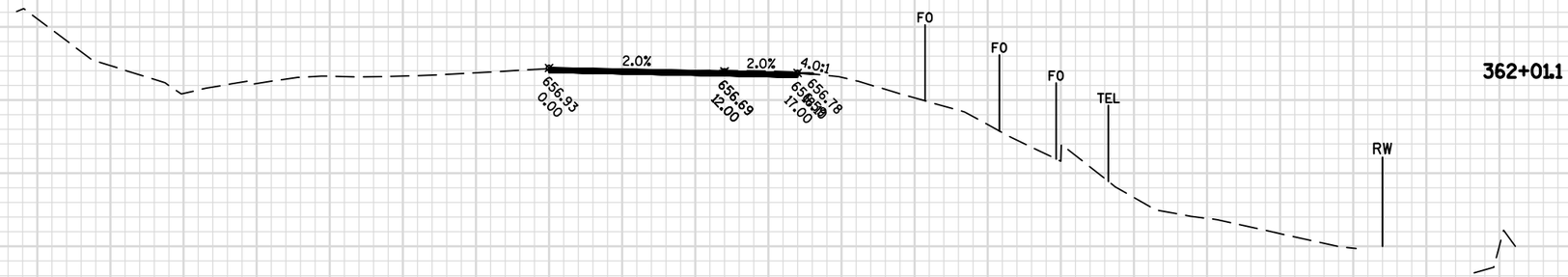
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG DATE : 2/12/2016 10:05 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090202_(9)

WISDOT/CADDS SHEET 49



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 201	E

STA. 362+01.1
END BARRIER SYSTEM GRADING SHAPING FINISHING RT



STA. 362+00.0
END BARRIER SYSTEM GRADING SHAPING FINISHING RT

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202_XS_355-362_INFO_ONLY.DWG
LAYOUT NAME - 090202_(11)
PLOTTED DATE : 2/12/2016 10:05 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

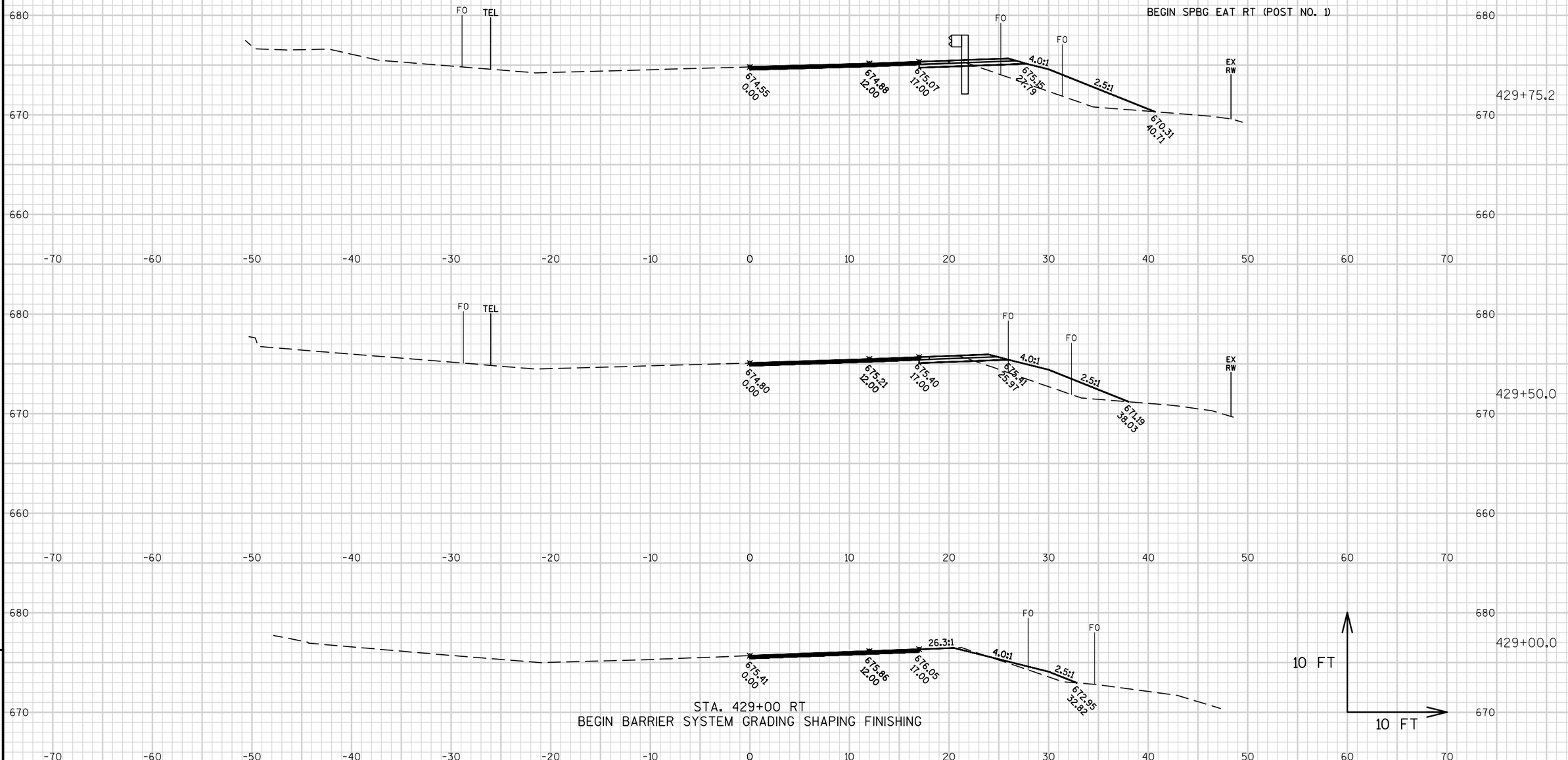
WISDOT/CADDS SHEET 49

NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.

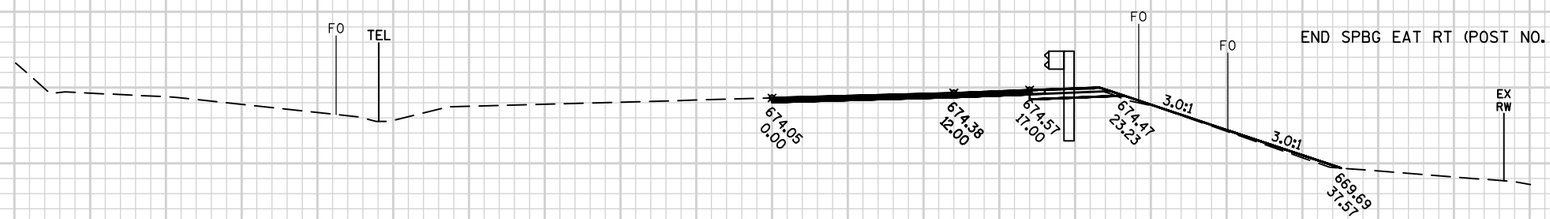


STA. 429+00 RT
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 203	E

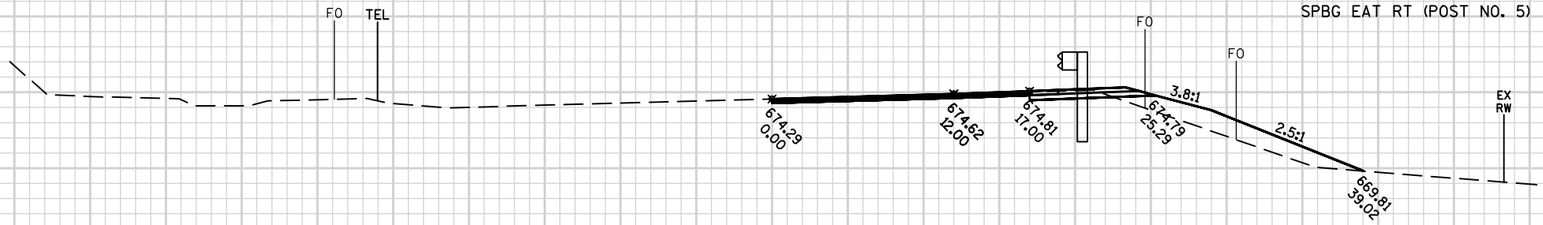
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT1
PLOT DATE : 1/17/2017 11:20 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

END SPBG EAT RT (POST NO. 9)

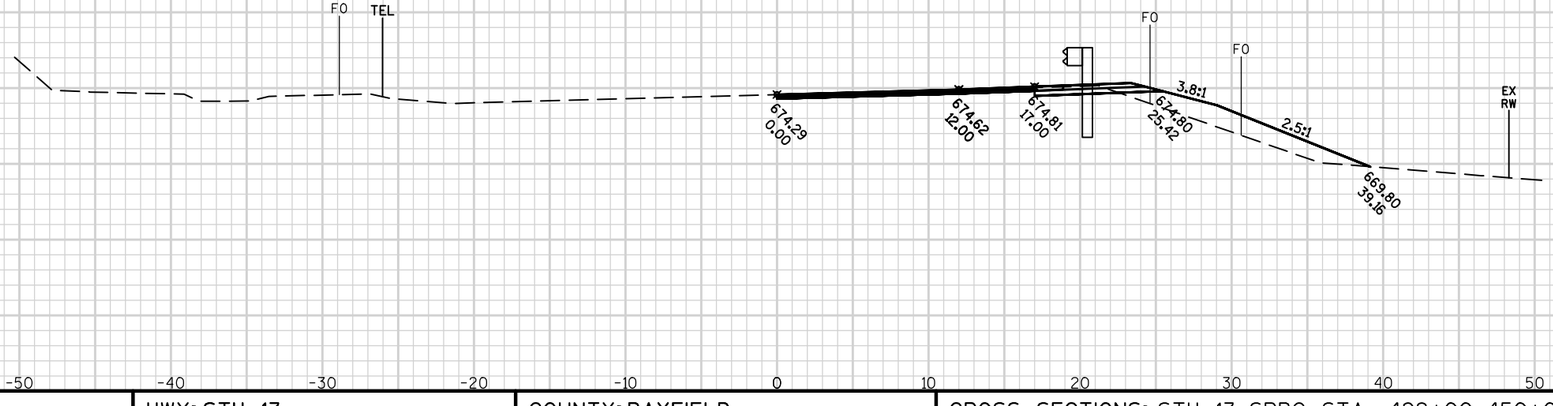


430+25.2

SPBG EAT RT (POST NO. 5)



430+00.2



430+00.0

9

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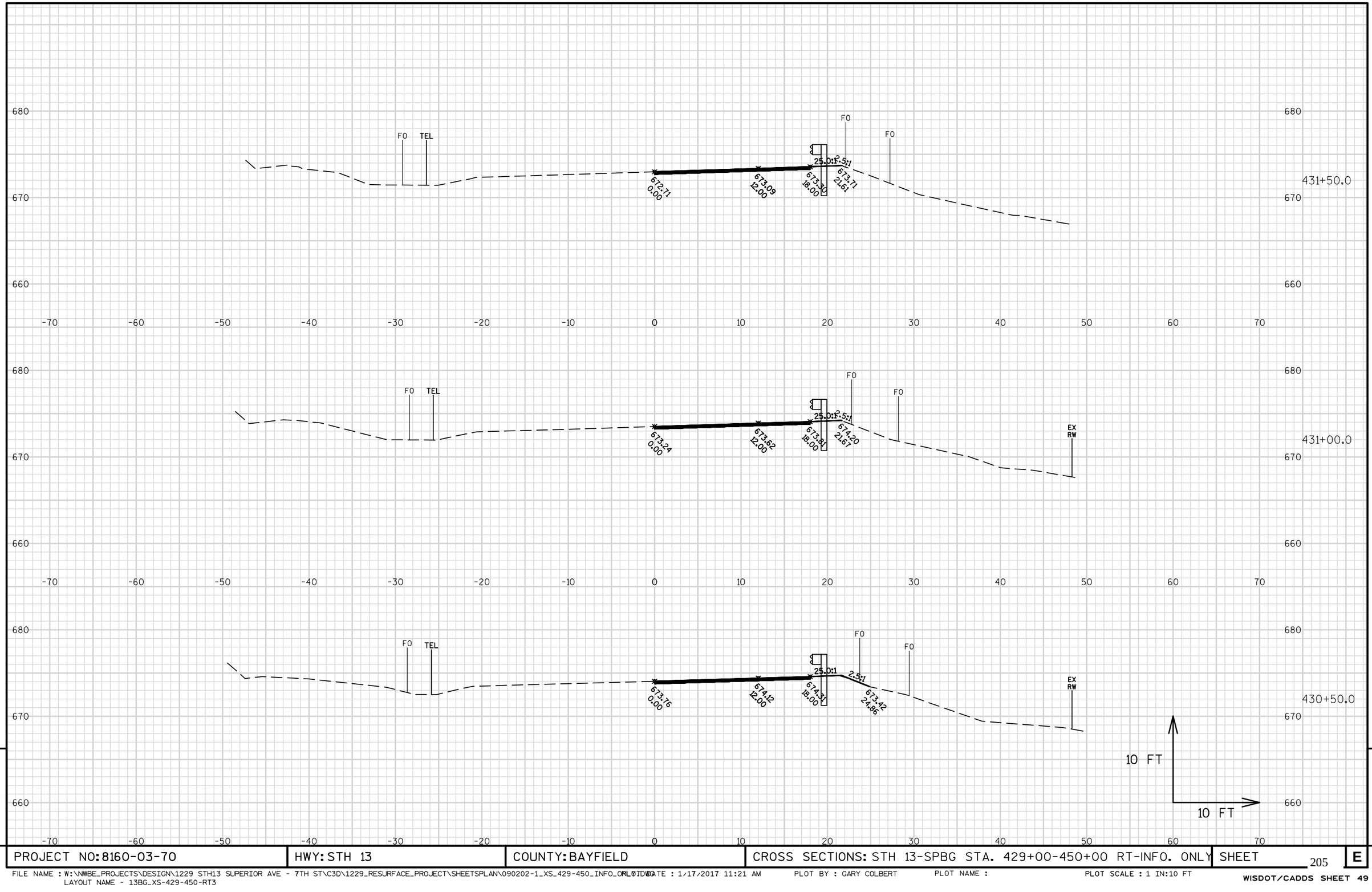
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 204	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ORL.DWG
LAYOUT NAME - 13BG_XS-429-450-RT2

PLOT DATE : 1/17/2017 11:20 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



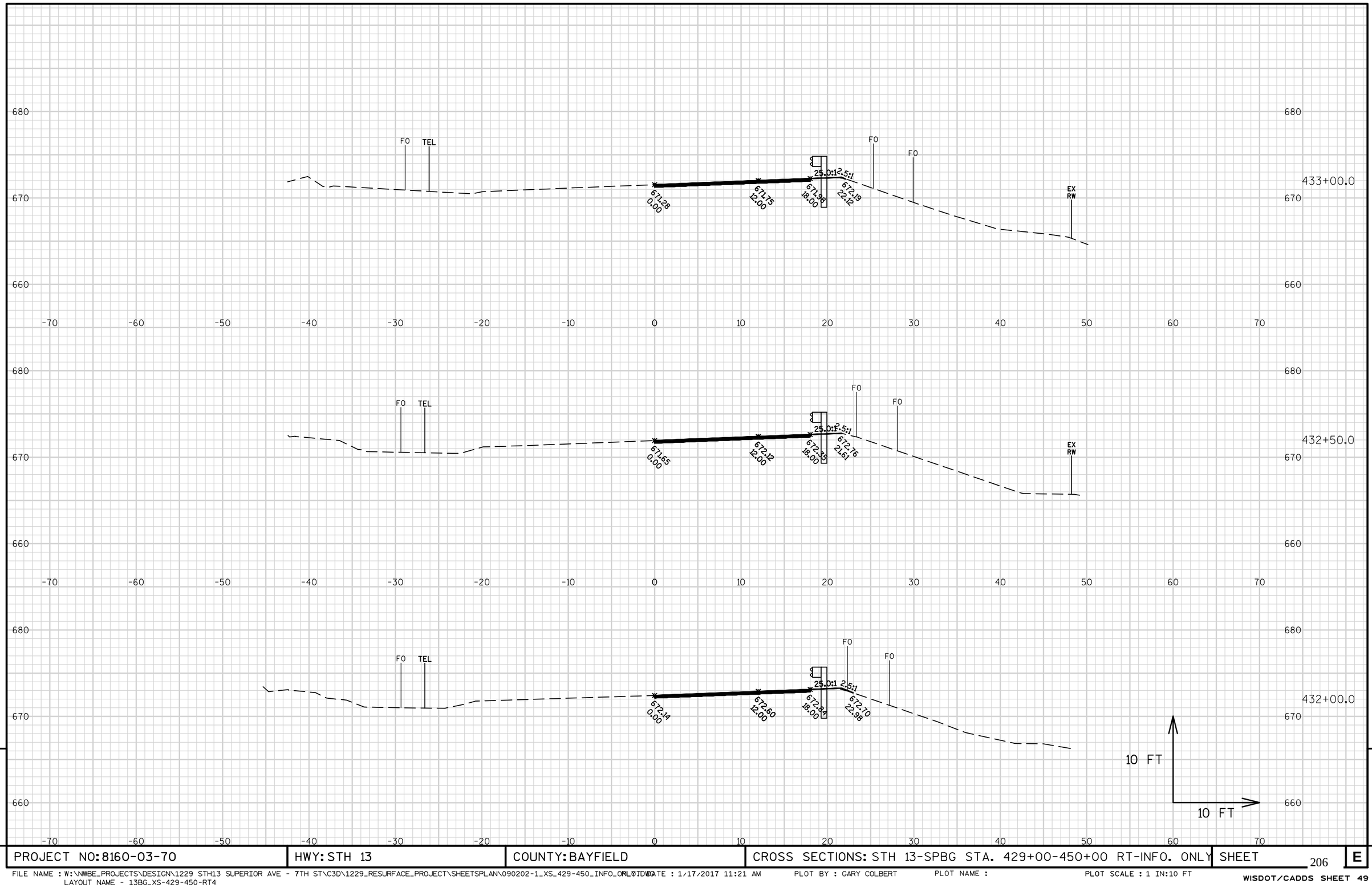
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 205	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT3
PLOT DATE : 1/17/2017 11:21 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 206	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ORIG.DWG
LAYOUT NAME - 13BG_XS-429-450-RT4

PLOTTED : DATE : 1/17/2017 11:21 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

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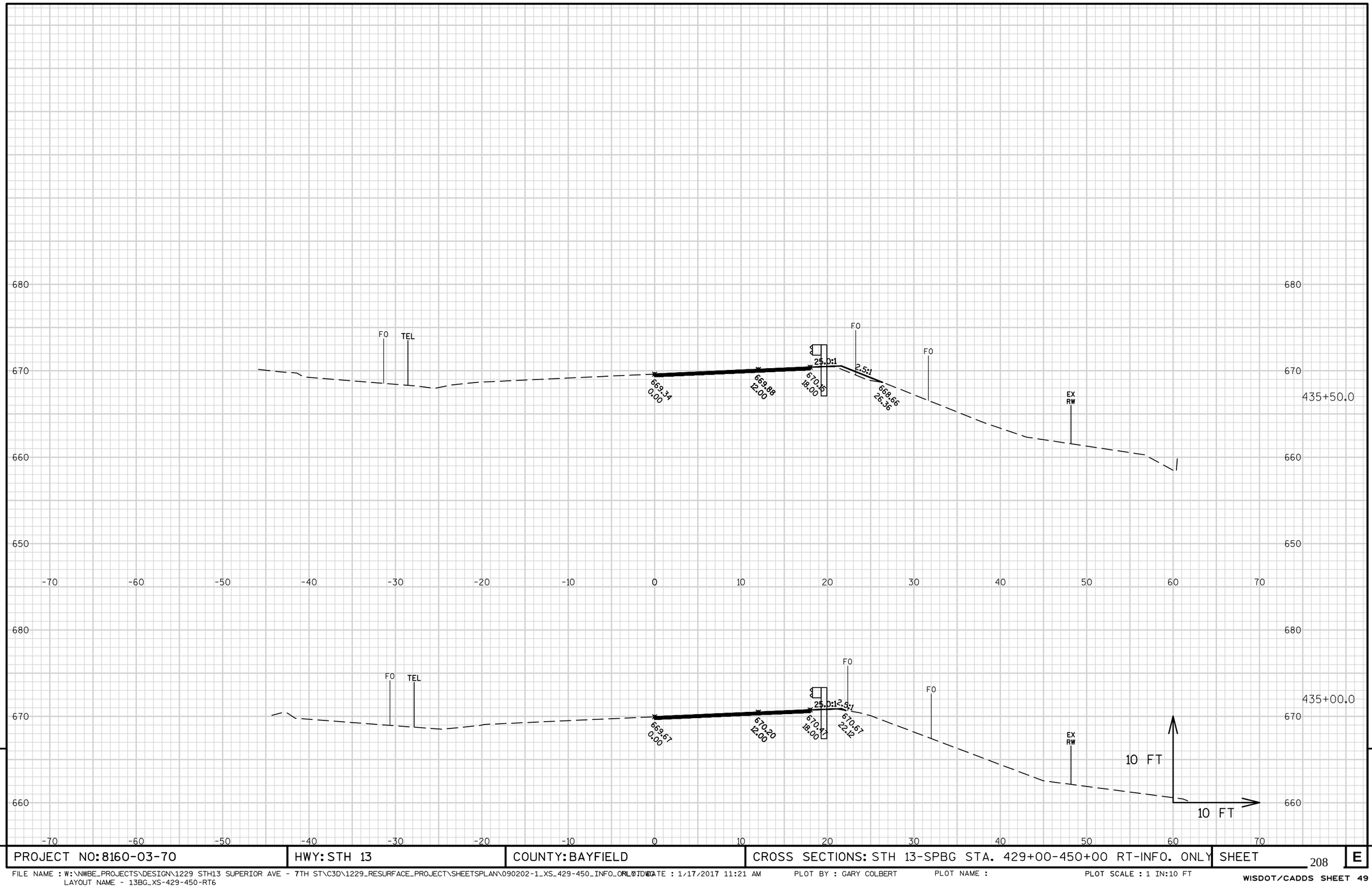
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 207	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ORL.DWG
LAYOUT NAME - 13BG_XS-429-450-RT5
PLOTTED DATE : 1/17/2017 11:21 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



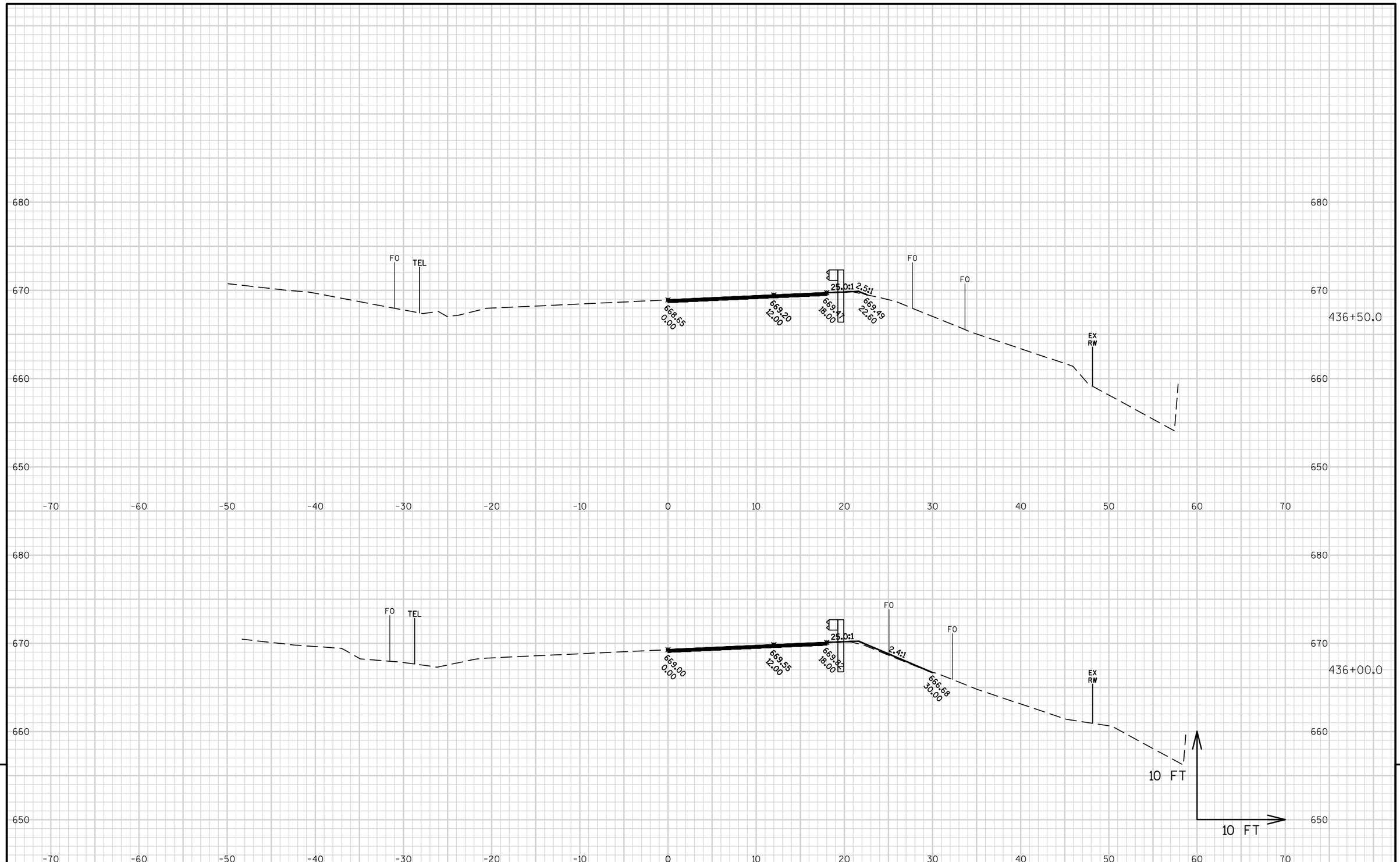
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 208	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT6

PLOTTED : 1/17/2017 11:21 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

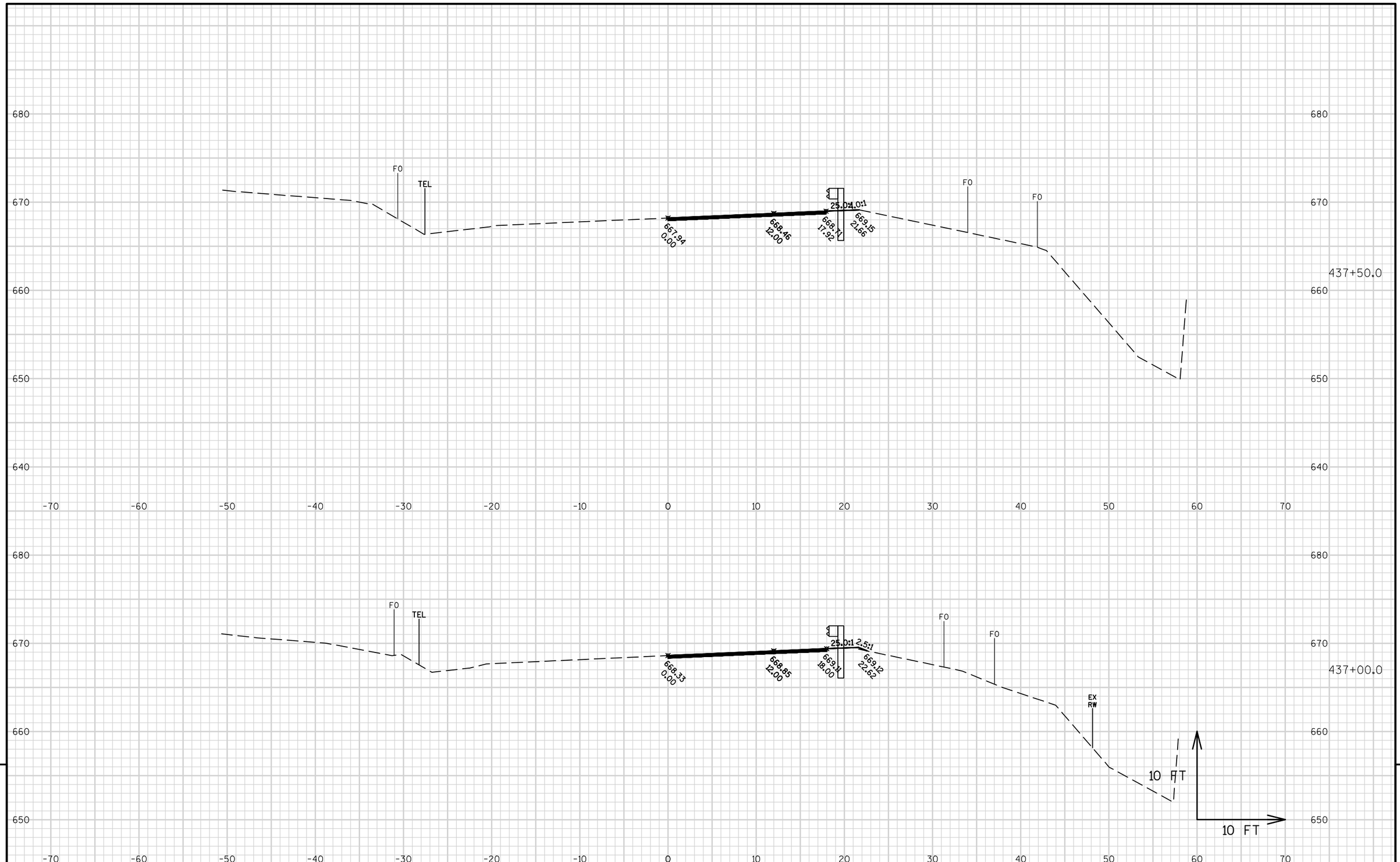


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 209	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:21 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT7

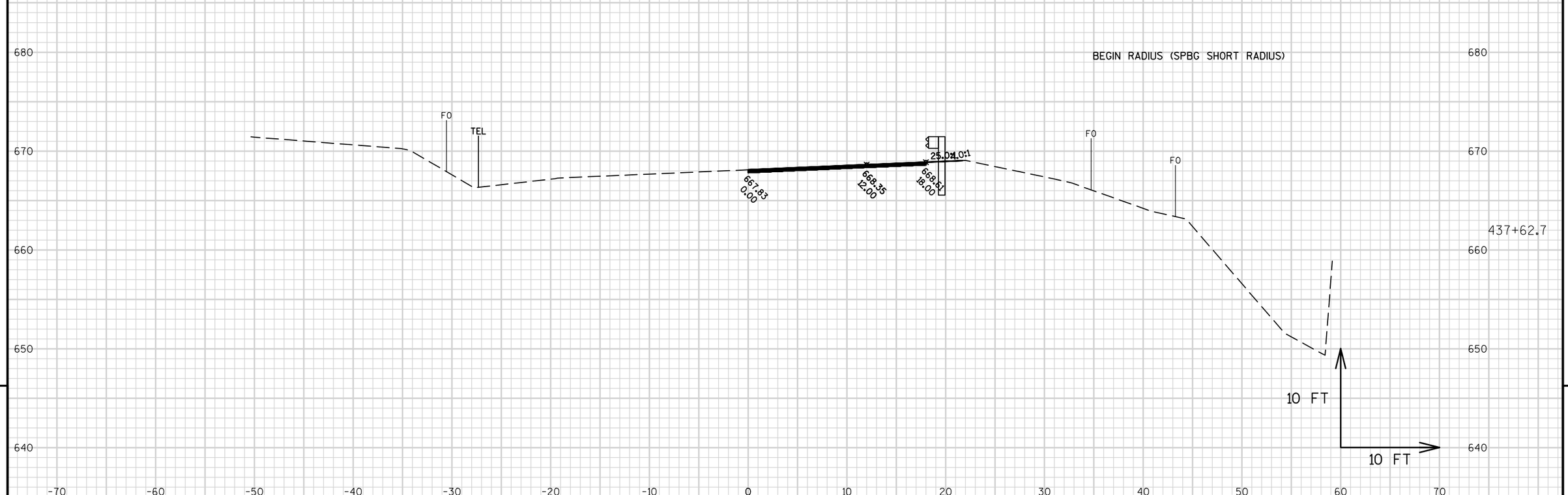


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 210	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT8
PLOT DATE : 1/17/2017 11:21 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 211	E

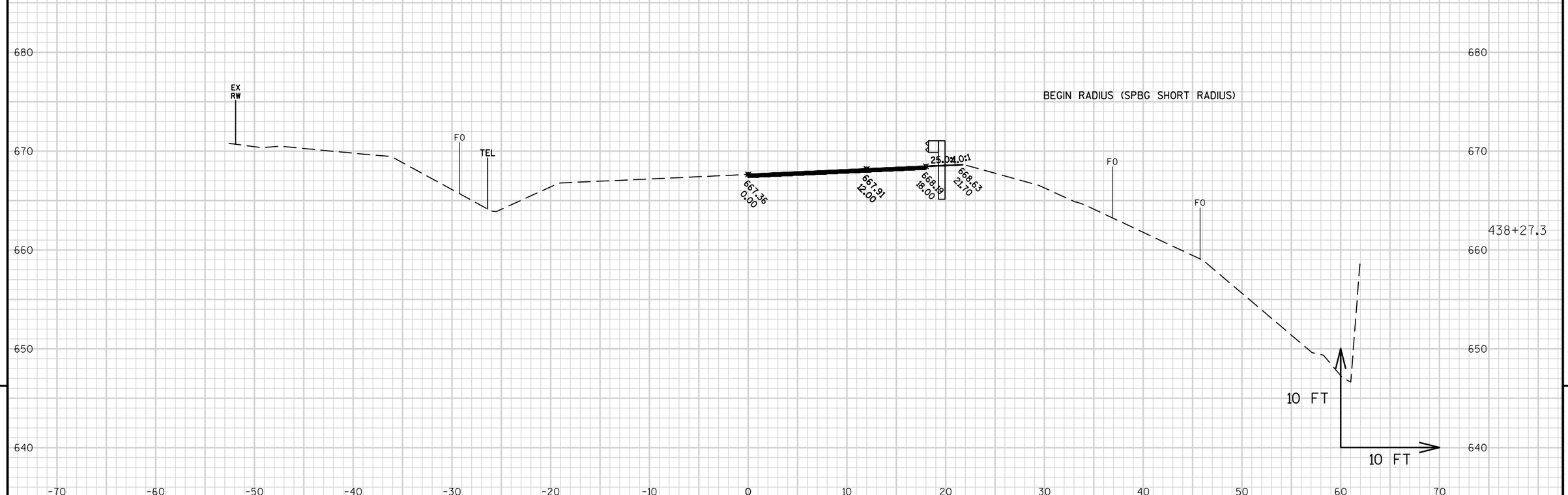
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:22 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT9

9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 212	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:22 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT10



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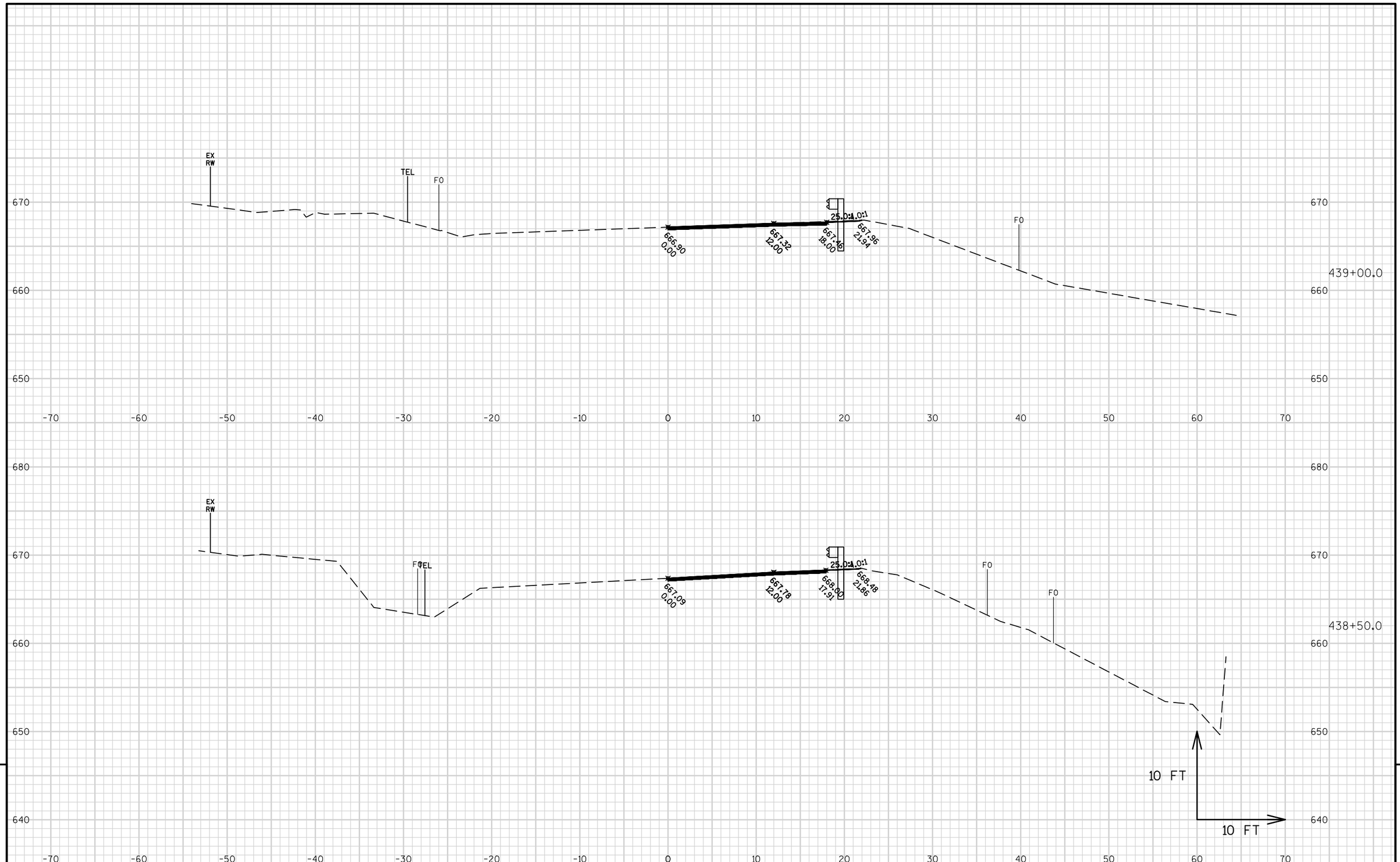
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 213	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ORIG.DWG
LAYOUT NAME - 13BG_XS-429-450-RT11
PLOTTED DATE : 1/17/2017 11:22 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



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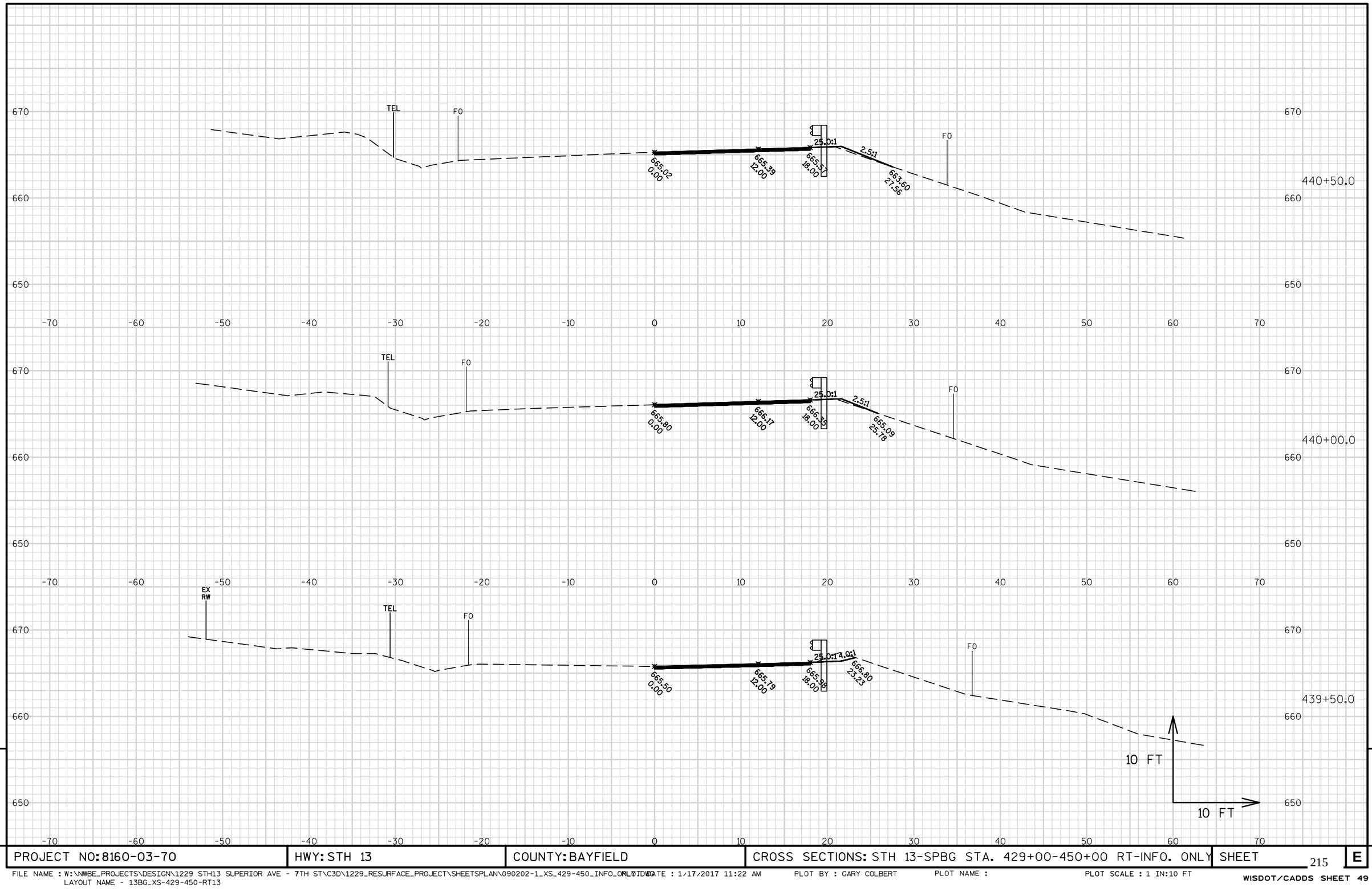
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 214	E

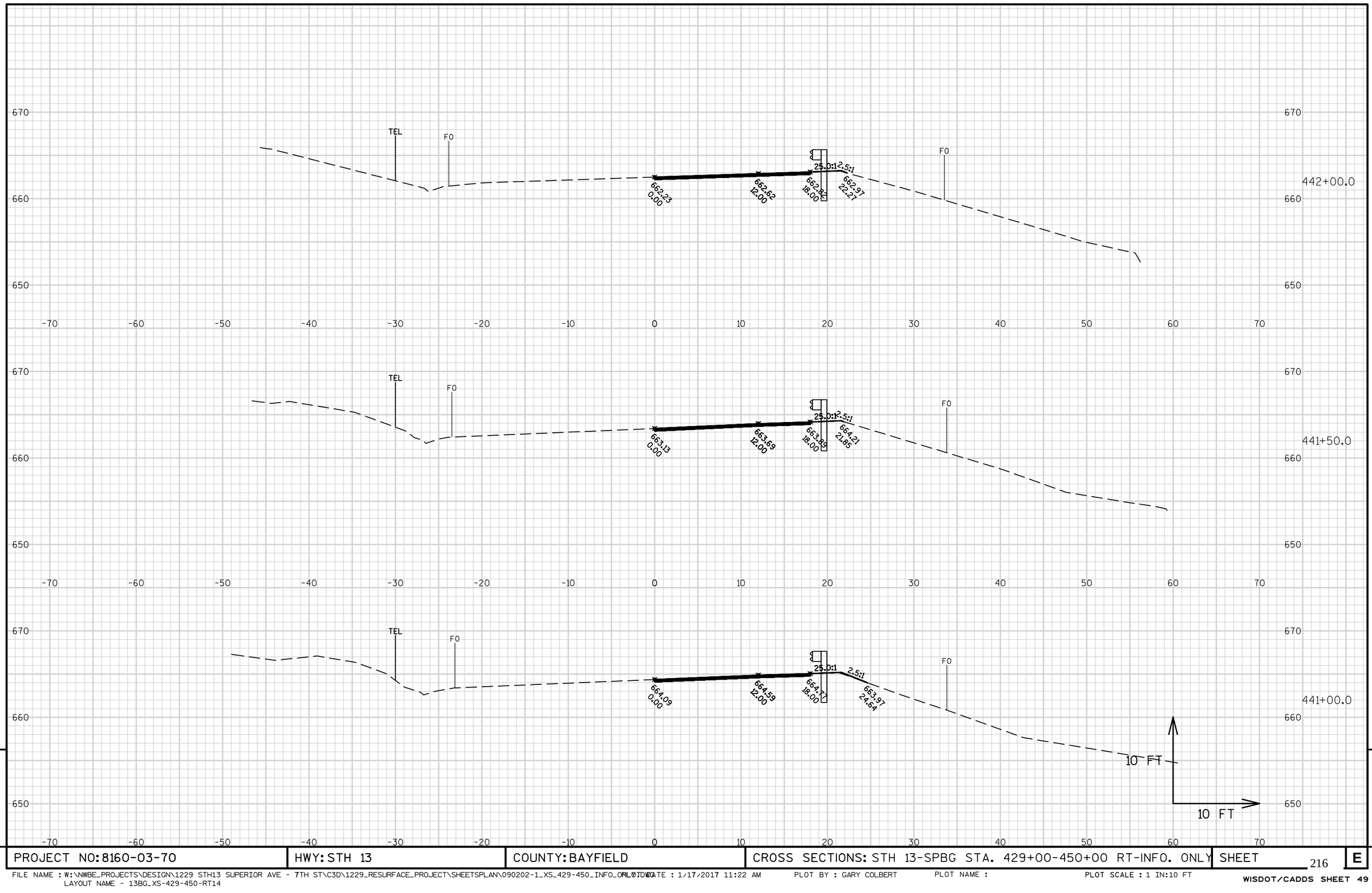
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:22 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT12



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 215	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:22 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT13

WISDOT/CADDS SHEET 49



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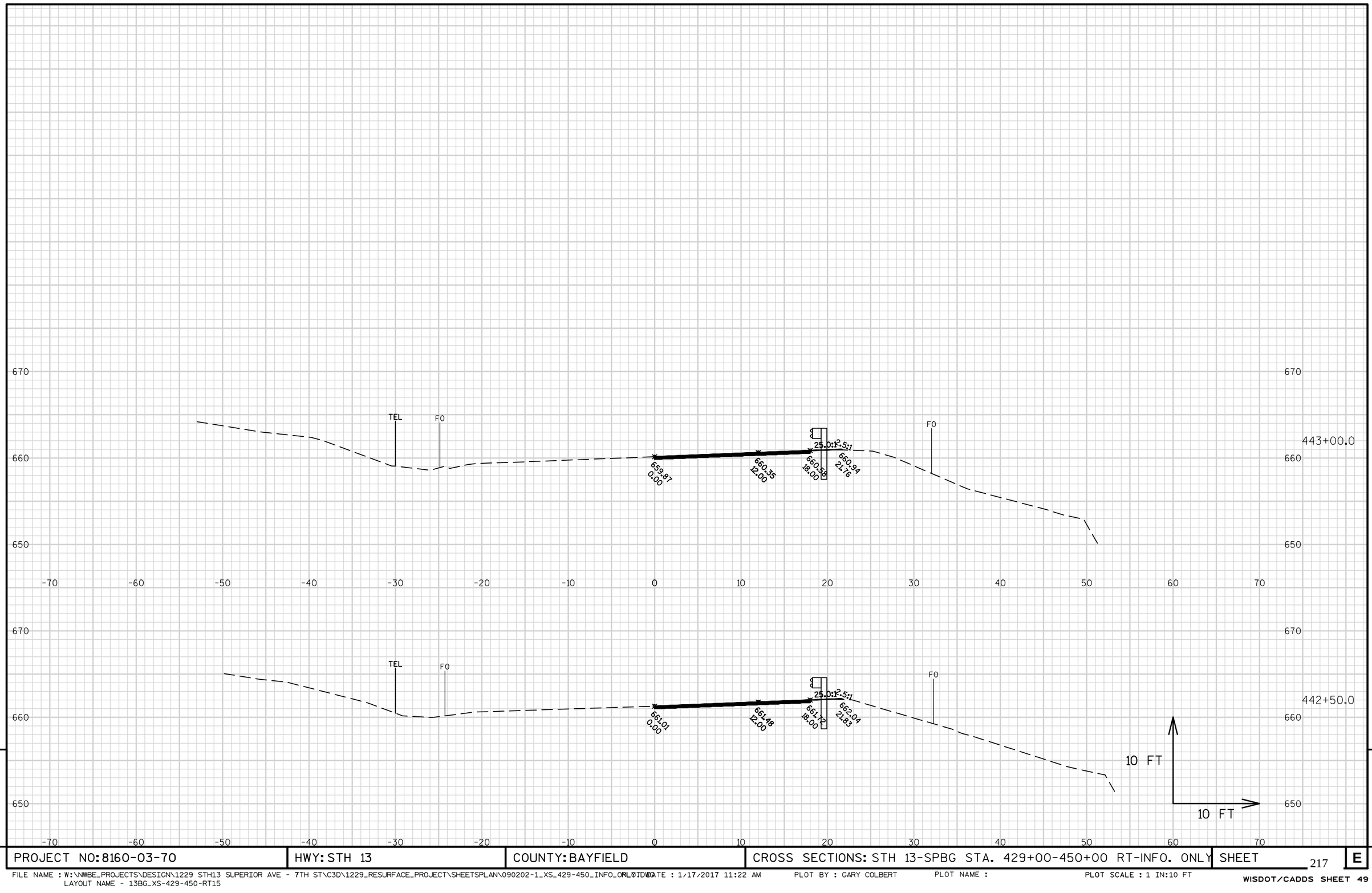
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 216	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT14
PLOT DATE : 1/17/2017 11:22 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



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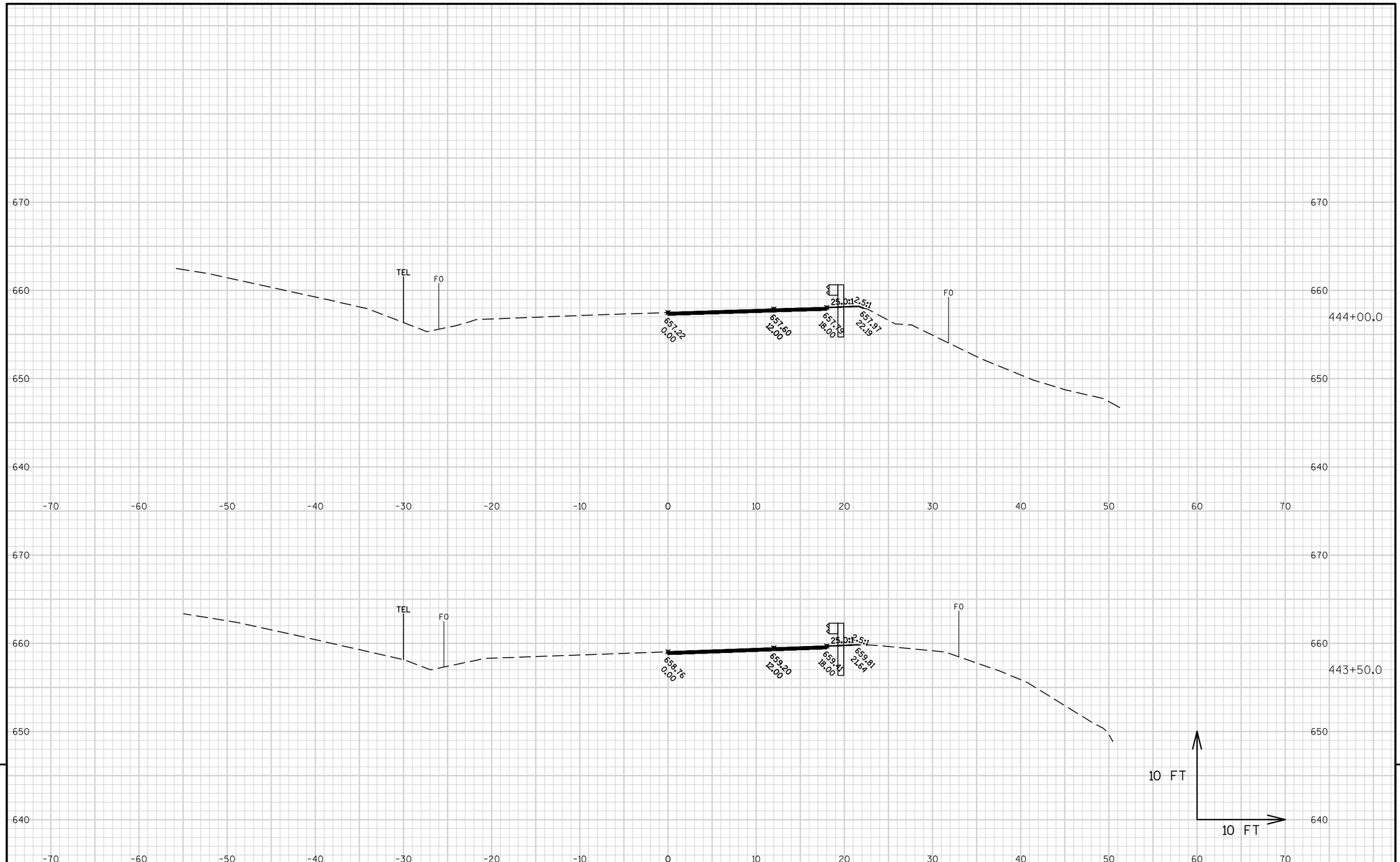
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 217	E

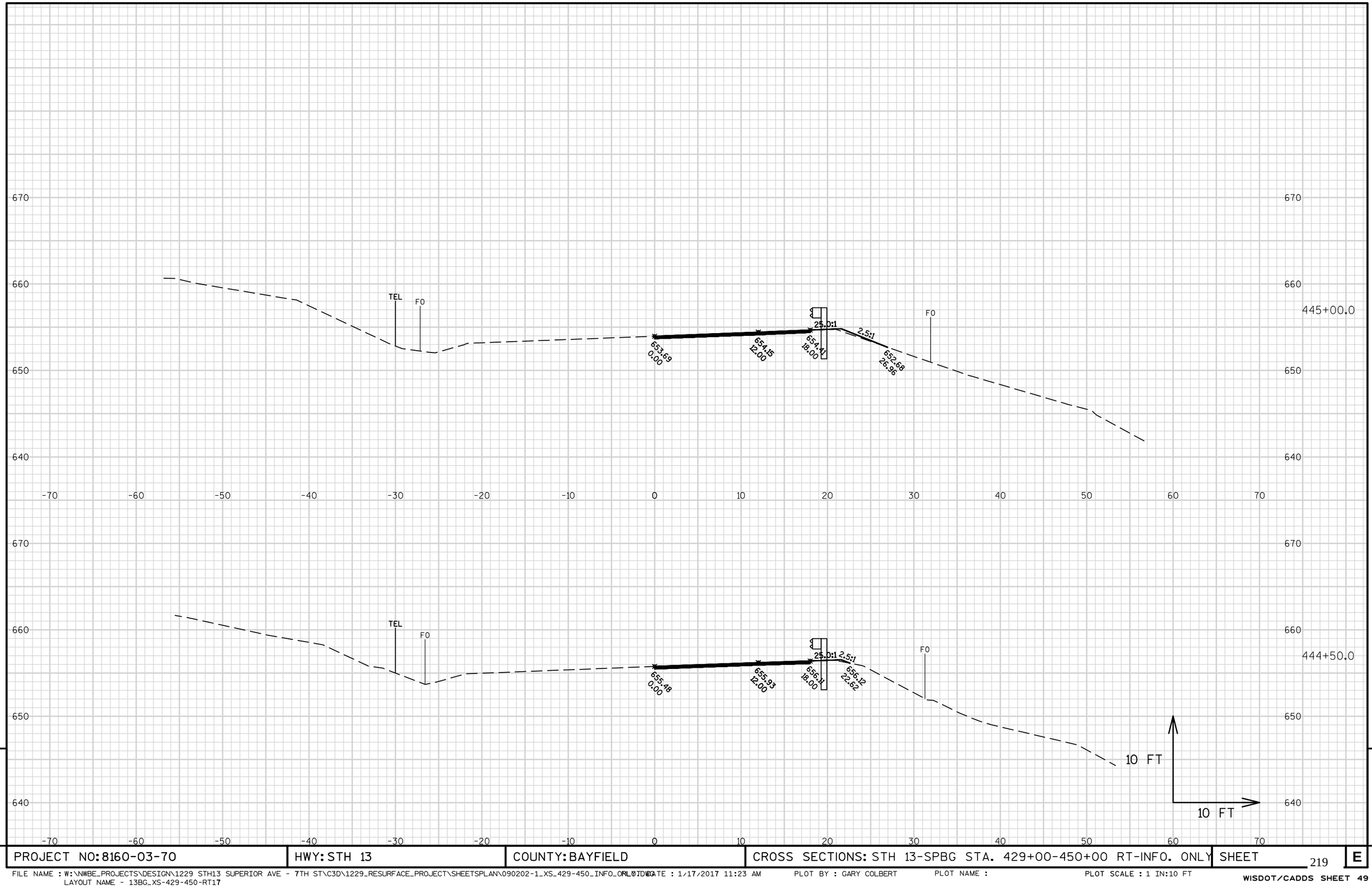


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 218	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:23 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT16



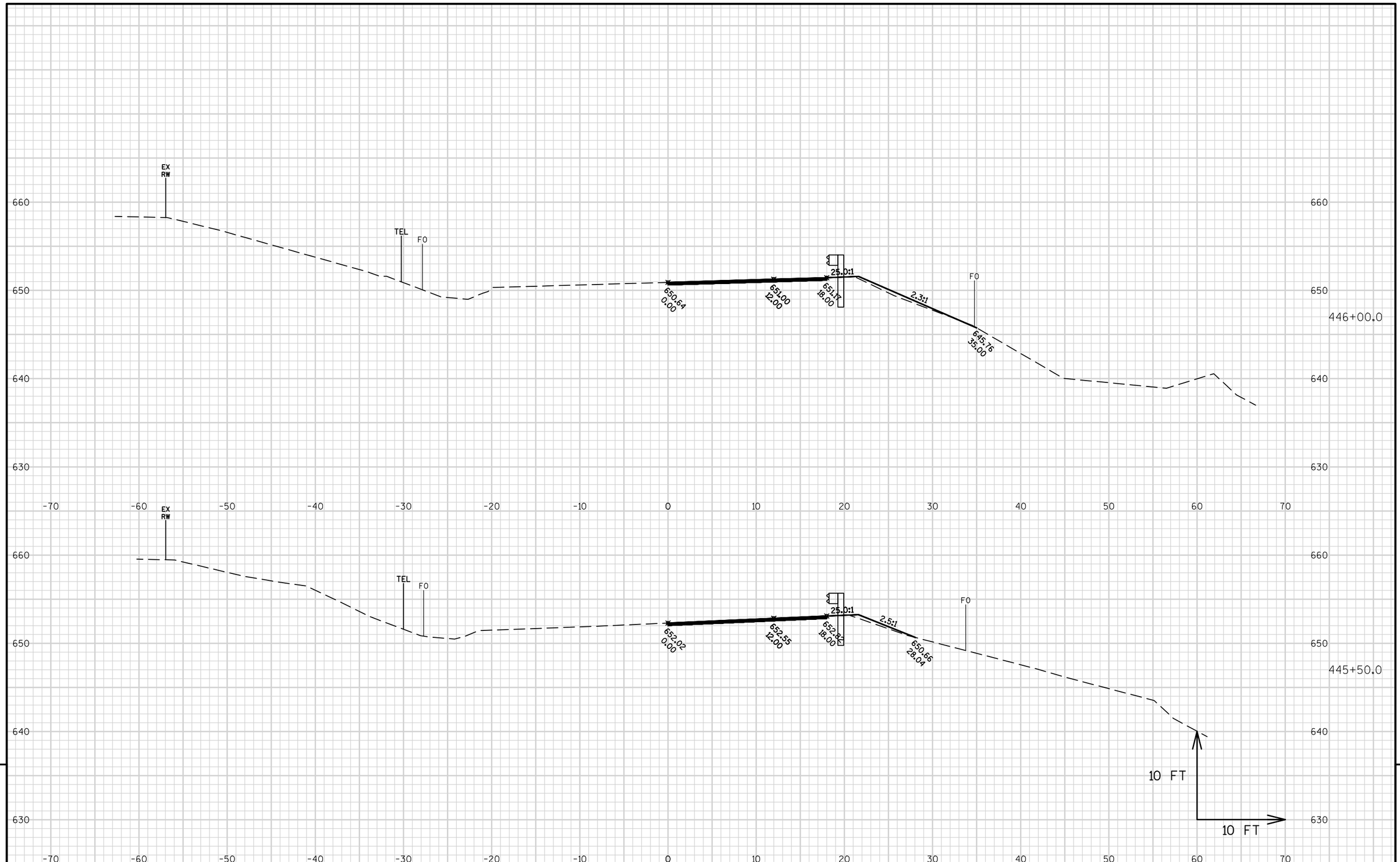
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 219	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:23 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT17

WISDOT/CADDS SHEET 49

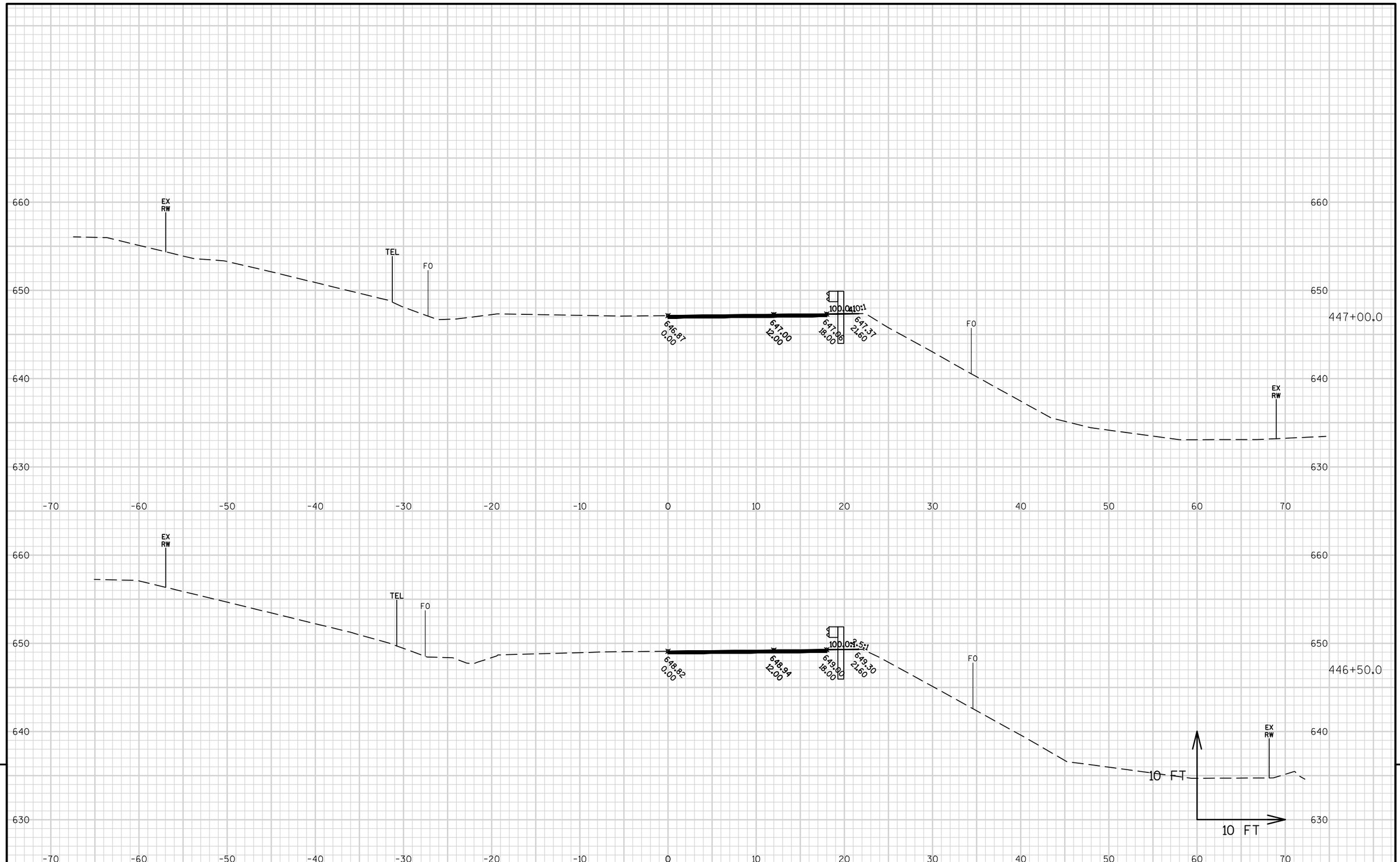


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 220	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT18
PLOTTED DATE : 1/17/2017 11:23 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



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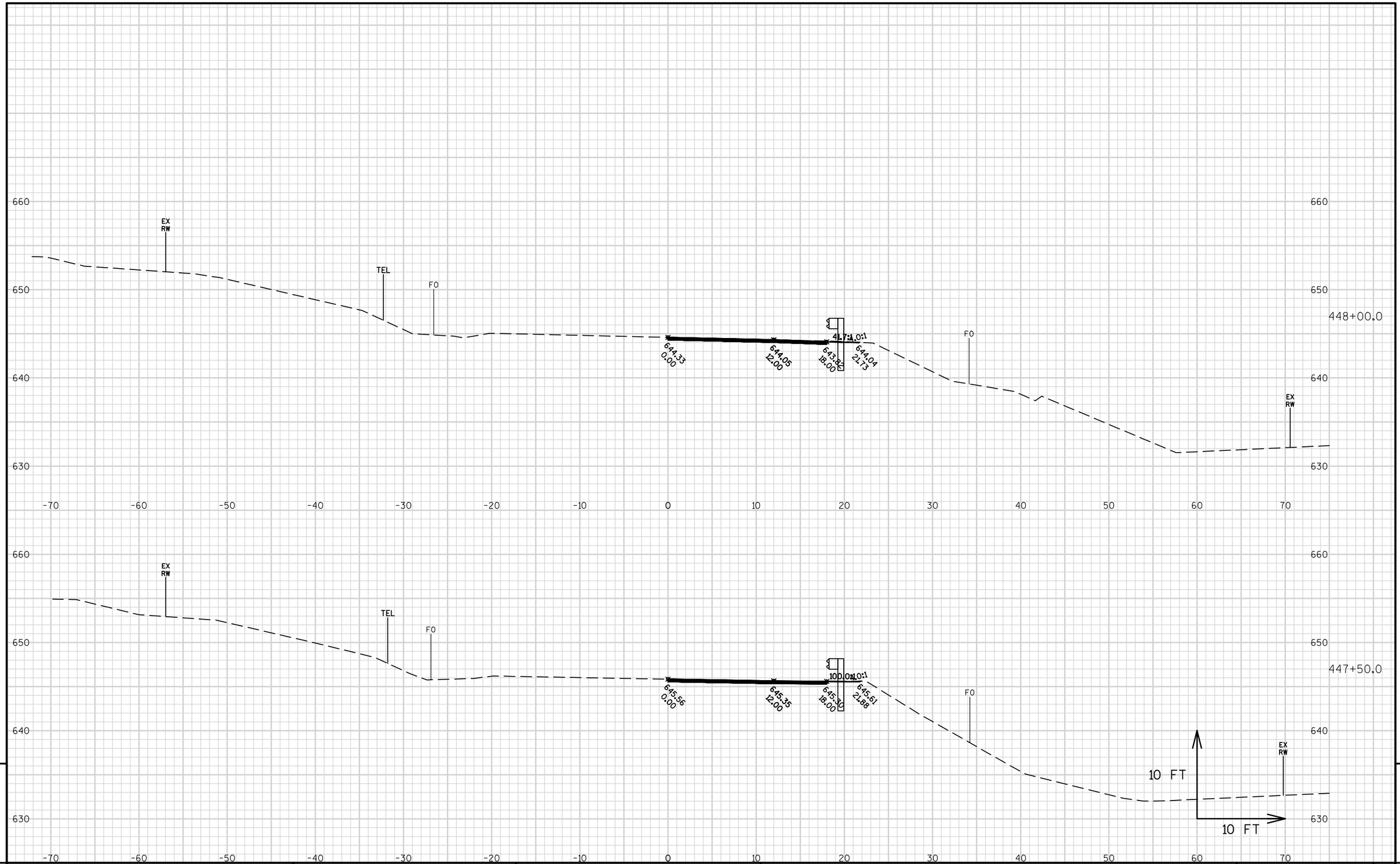
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 221	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT19
PLOT DATE : 1/17/2017 11:23 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



9

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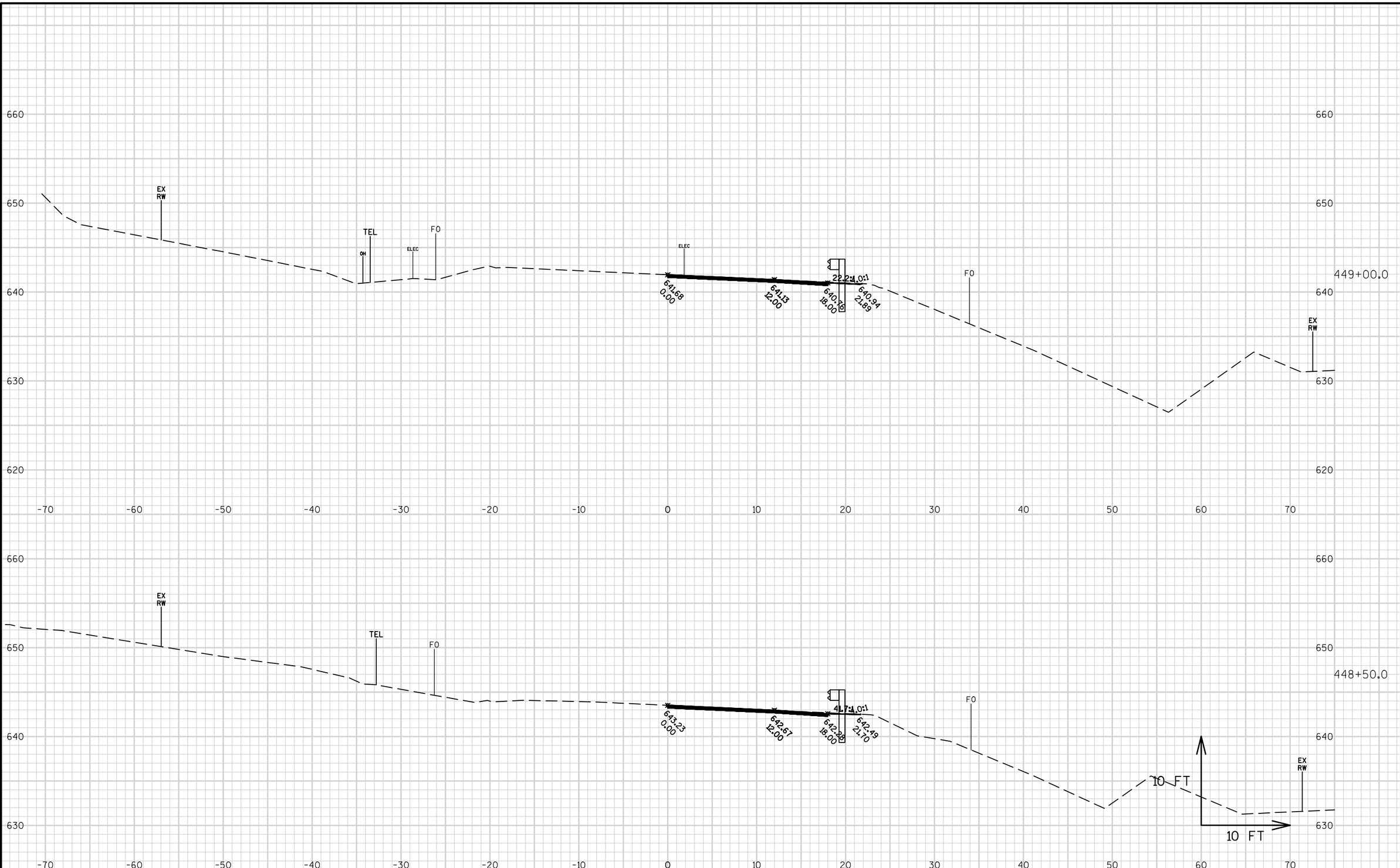
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 222	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG PLOT DATE : 1/17/2017 11:23 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-429-450-RT20



9

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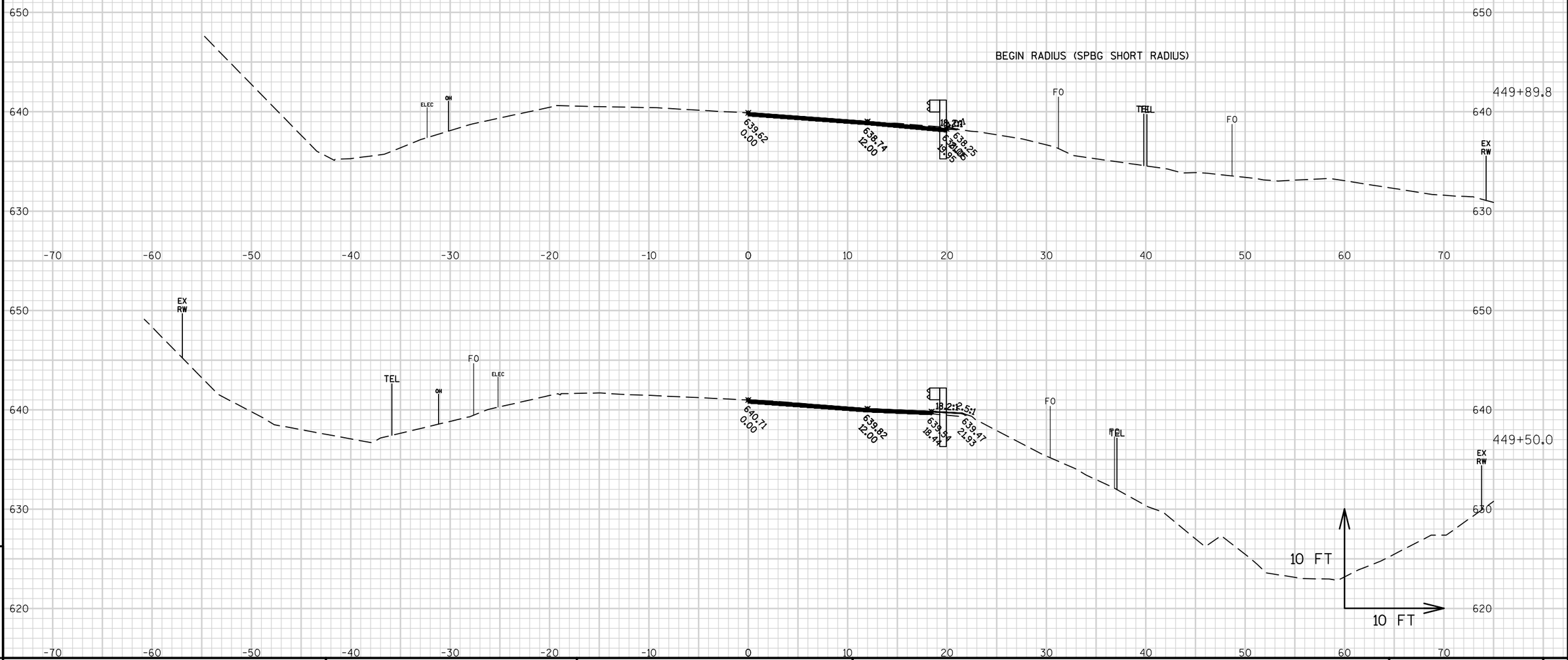
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 223	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT21
PLOTTED : 1/17/2017 11:23 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

STA. 450+06 RT
END BARRIER SYSTEM GRADING SHAPING FINISHING



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG STA. 429+00-450+00 RT-INFO. ONLY	SHEET 224	E

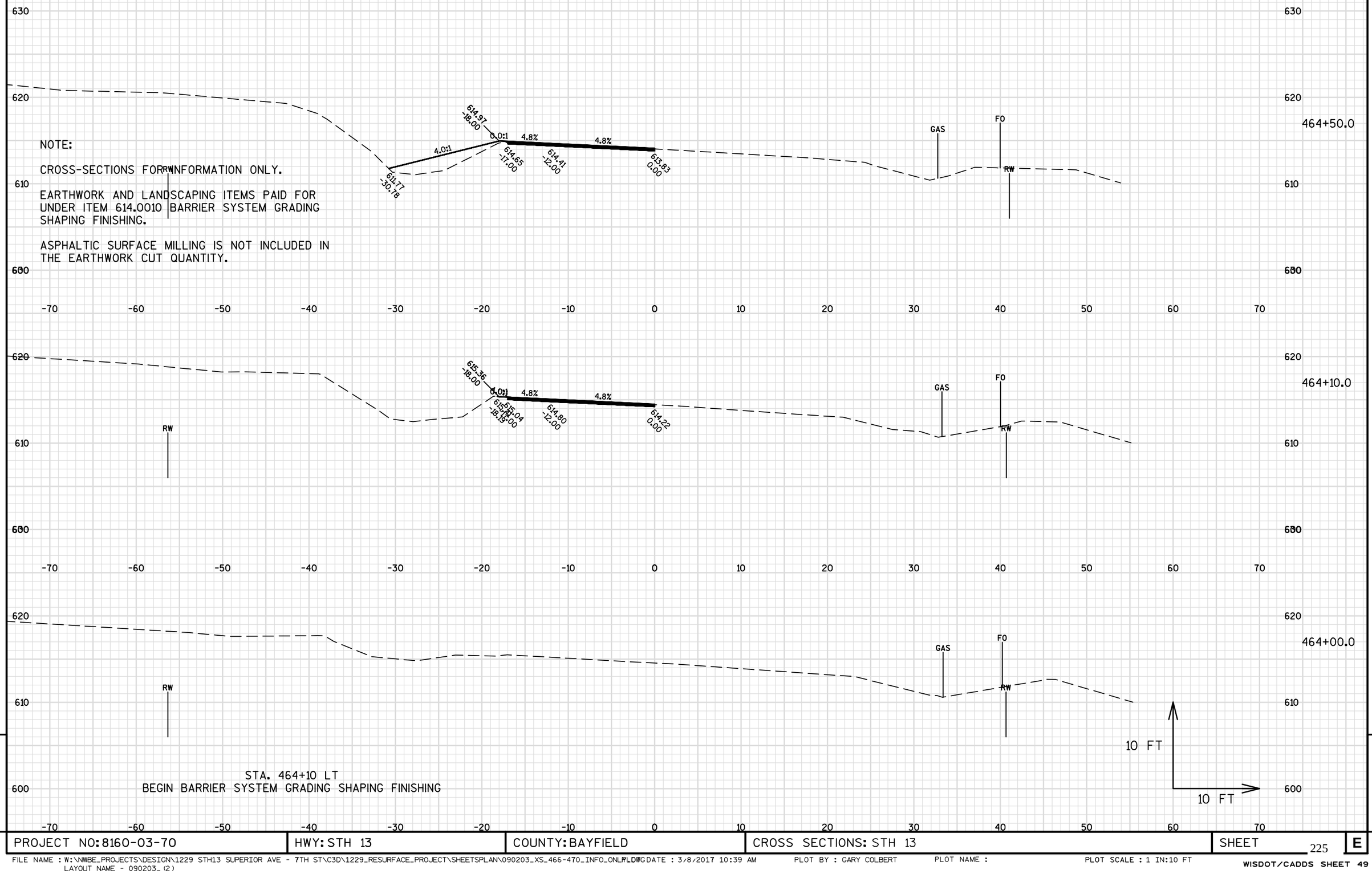
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090202-1_XS_429-450_INFO_ONLY.DWG
LAYOUT NAME - 13BG_XS-429-450-RT22
PLOTDATE : 1/17/2017 11:23 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.



STA. 464+10 LT
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING

9

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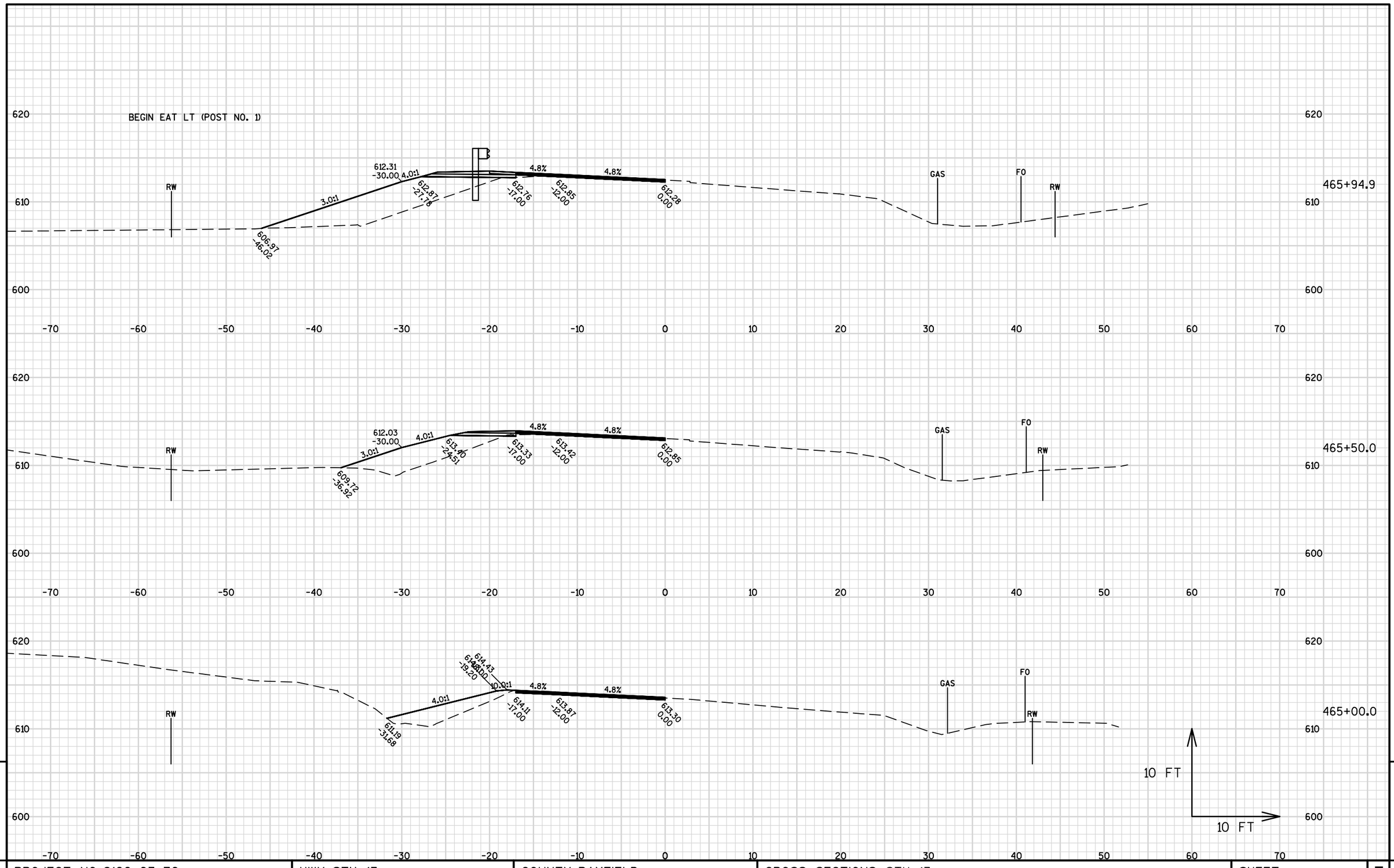
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 225	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(2)
PLOTTED DATE : 3/8/2017 10:39 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49



9

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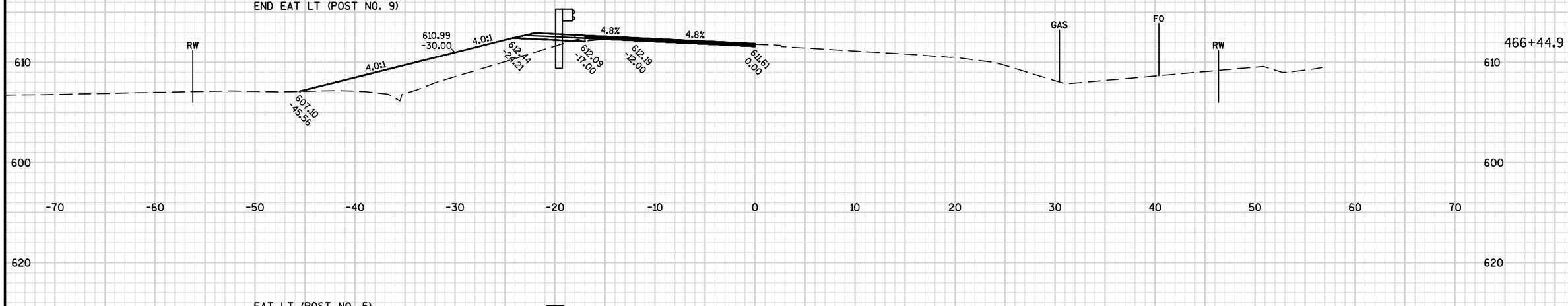
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 226	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(3)
PLOTTED DATE : 1/17/2017 11:31 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

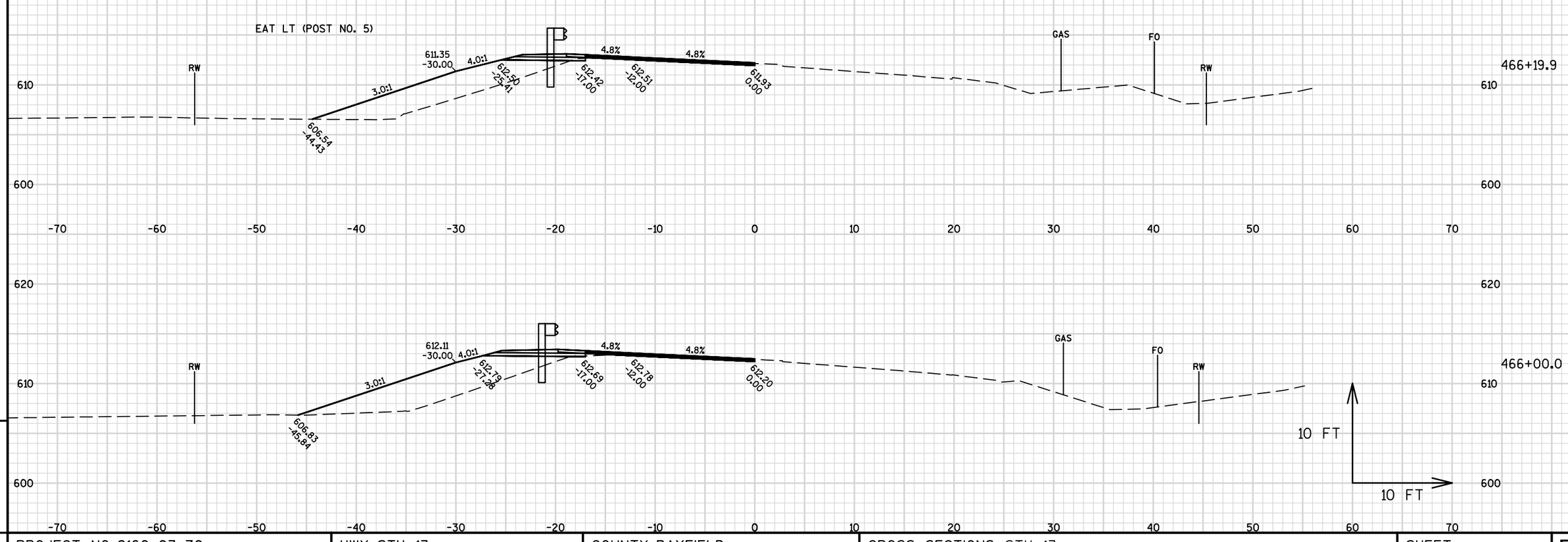
WISDOT/CADDS SHEET 49

END EAT LT (POST NO. 9)



466+44.9

EAT LT (POST NO. 5)



466+19.9

466+00.0

9

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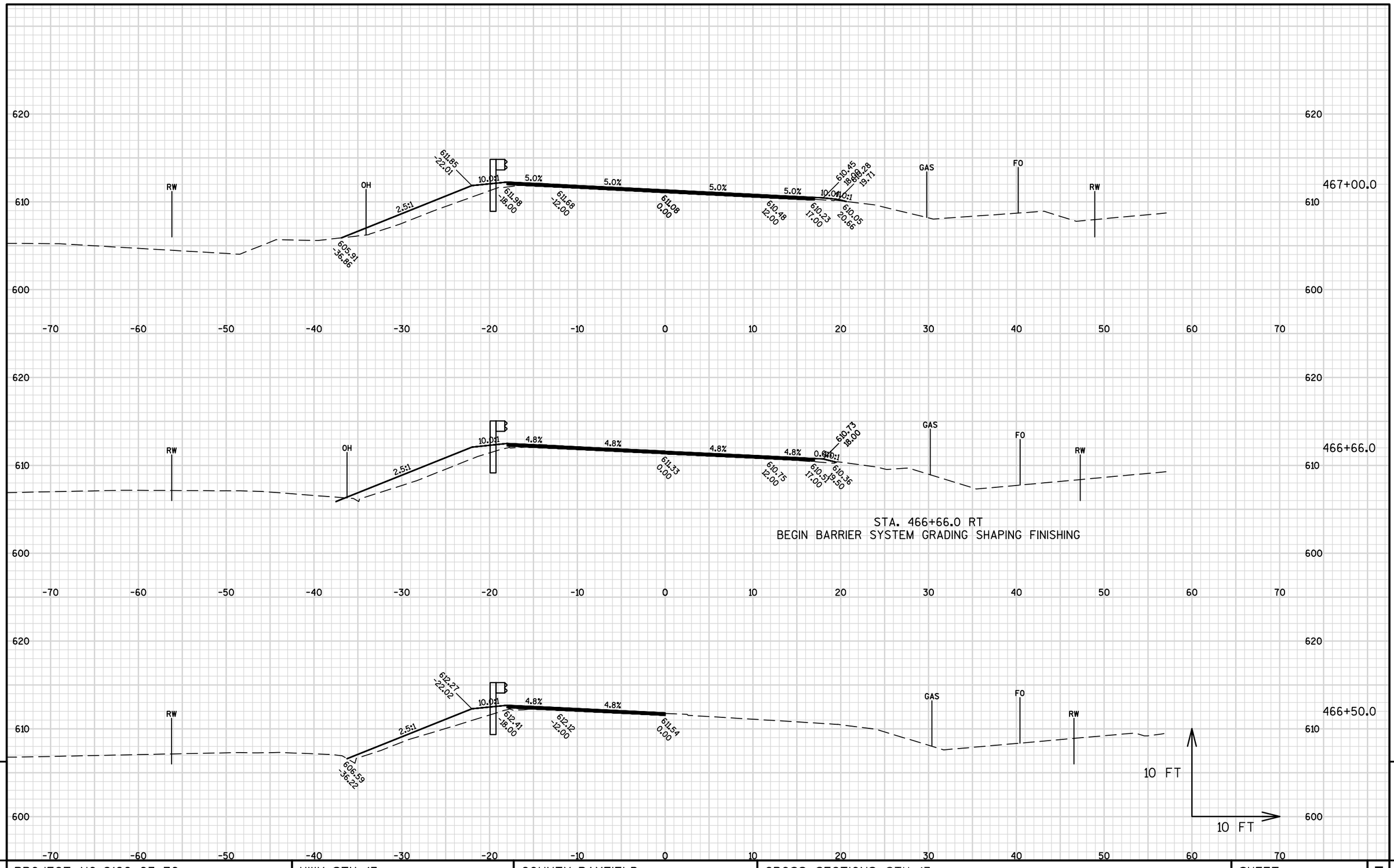
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 227	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(4)

PLOTTED DATE : 1/17/2017 11:31 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



STA. 466+66.0 RT
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING

9

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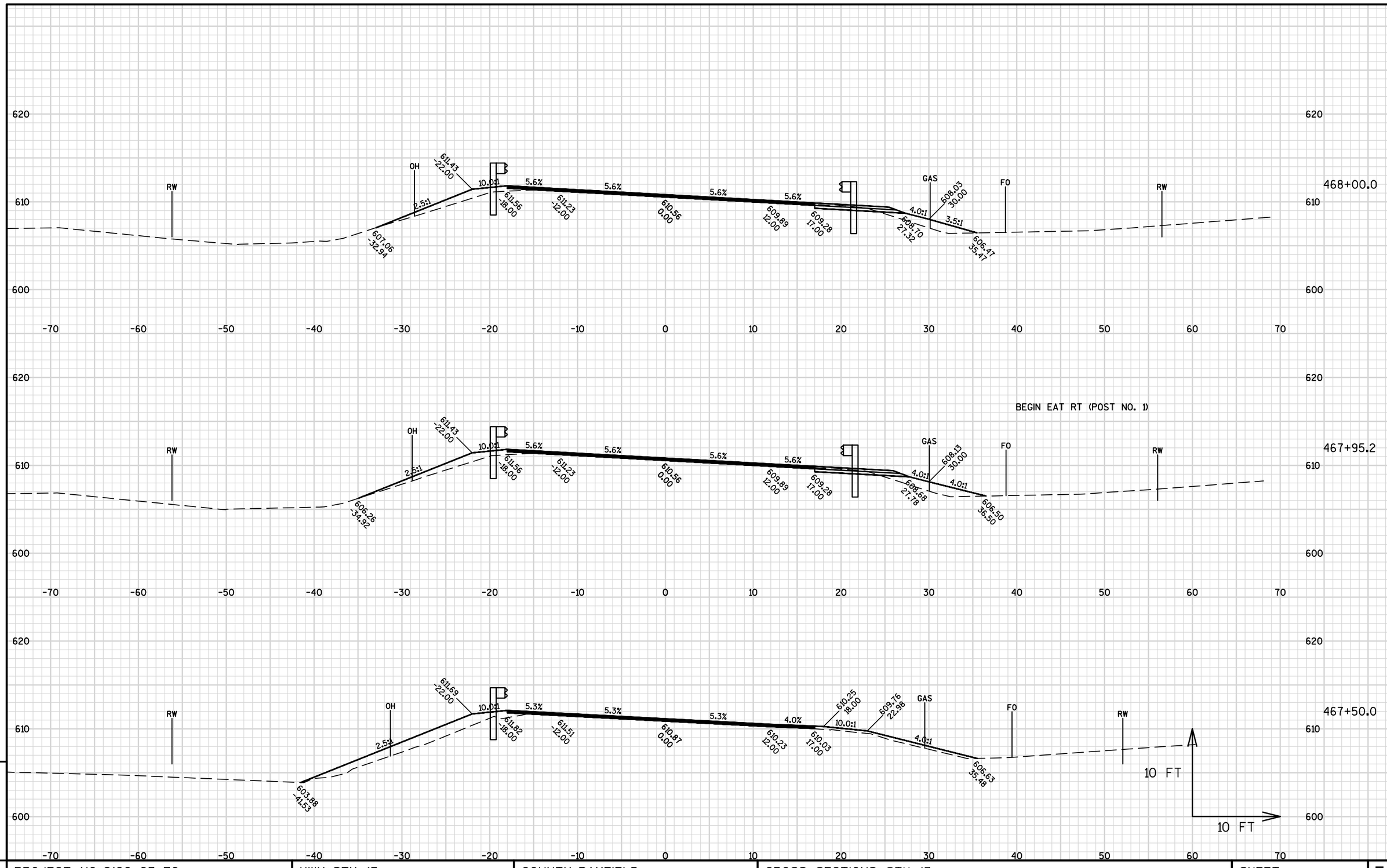
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 228	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(5)
PLOTTED DATE : 1/17/2017 11:31 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



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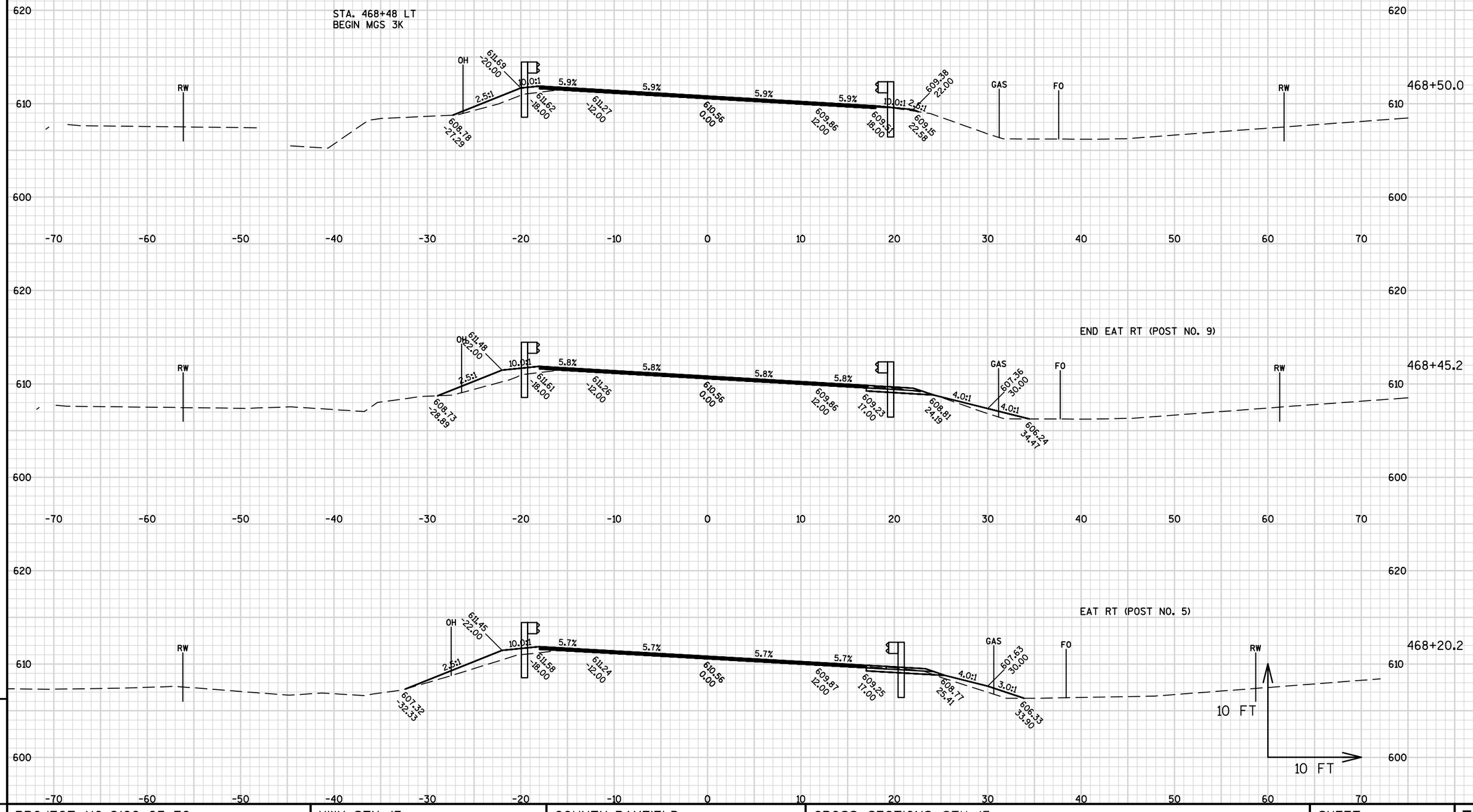
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 229	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(6)
PLOTTED DATE : 1/17/2017 11:32 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



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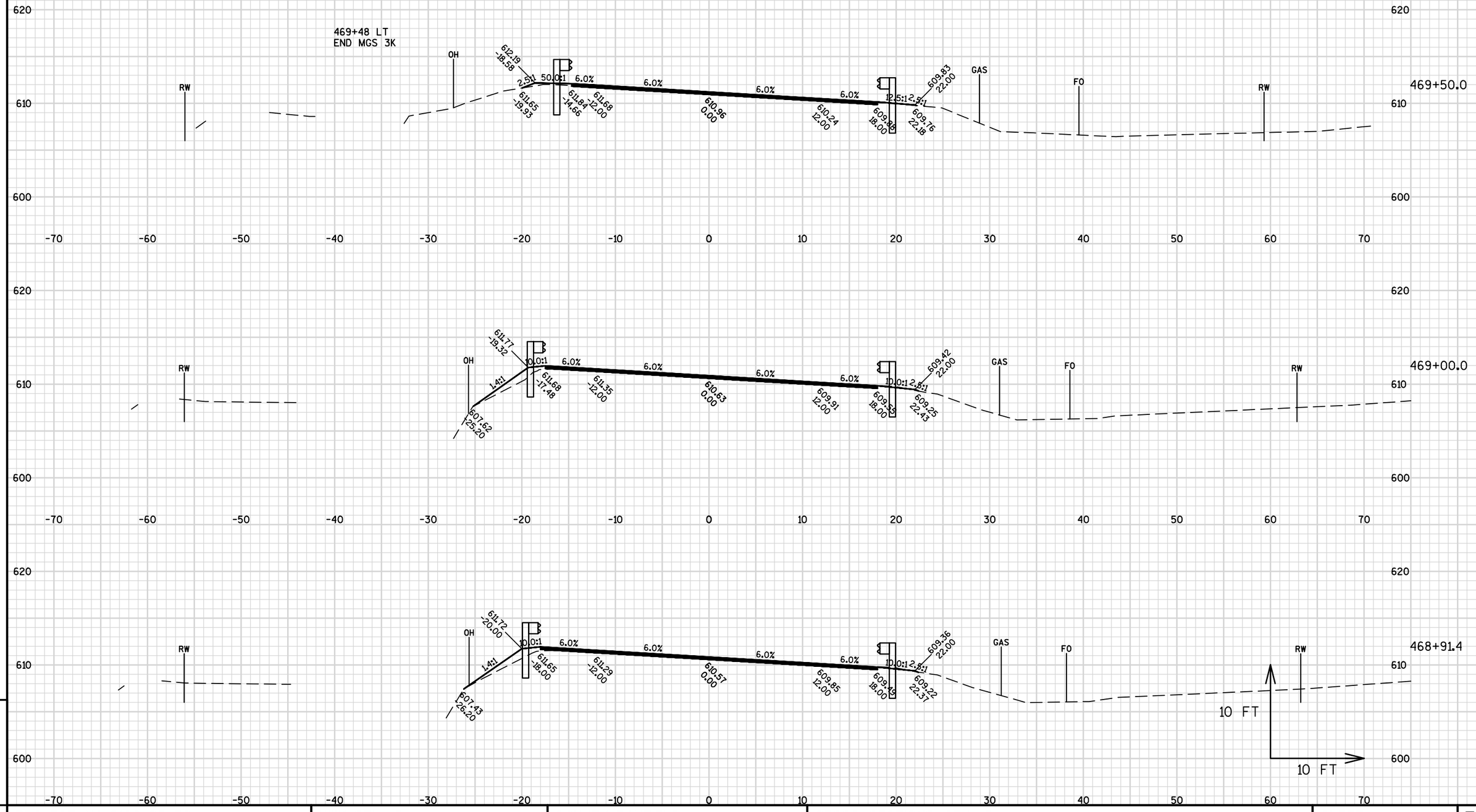
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 230	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(7)
PLOTTED DATE : 1/17/2017 11:32 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

469+48 LT
END MGS 3K

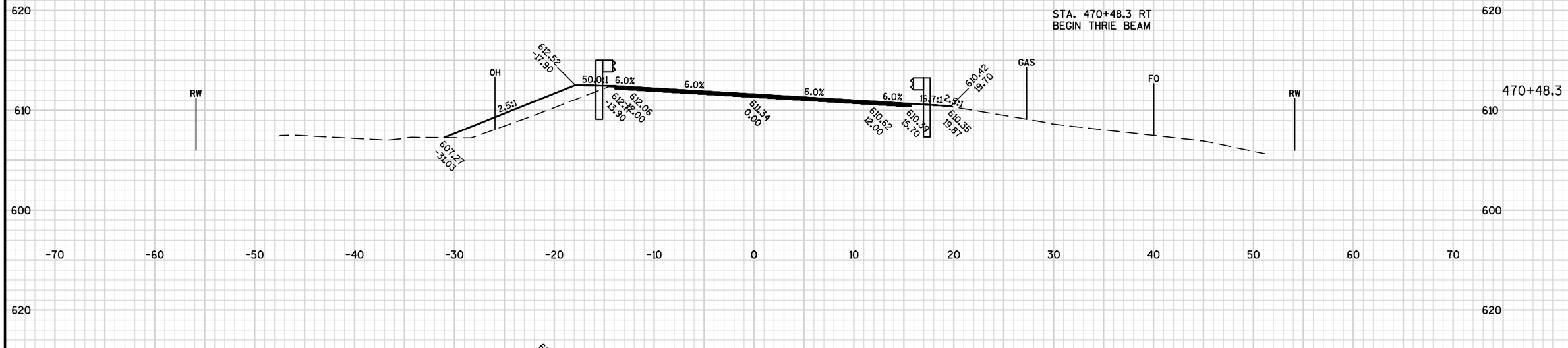


PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 231	E

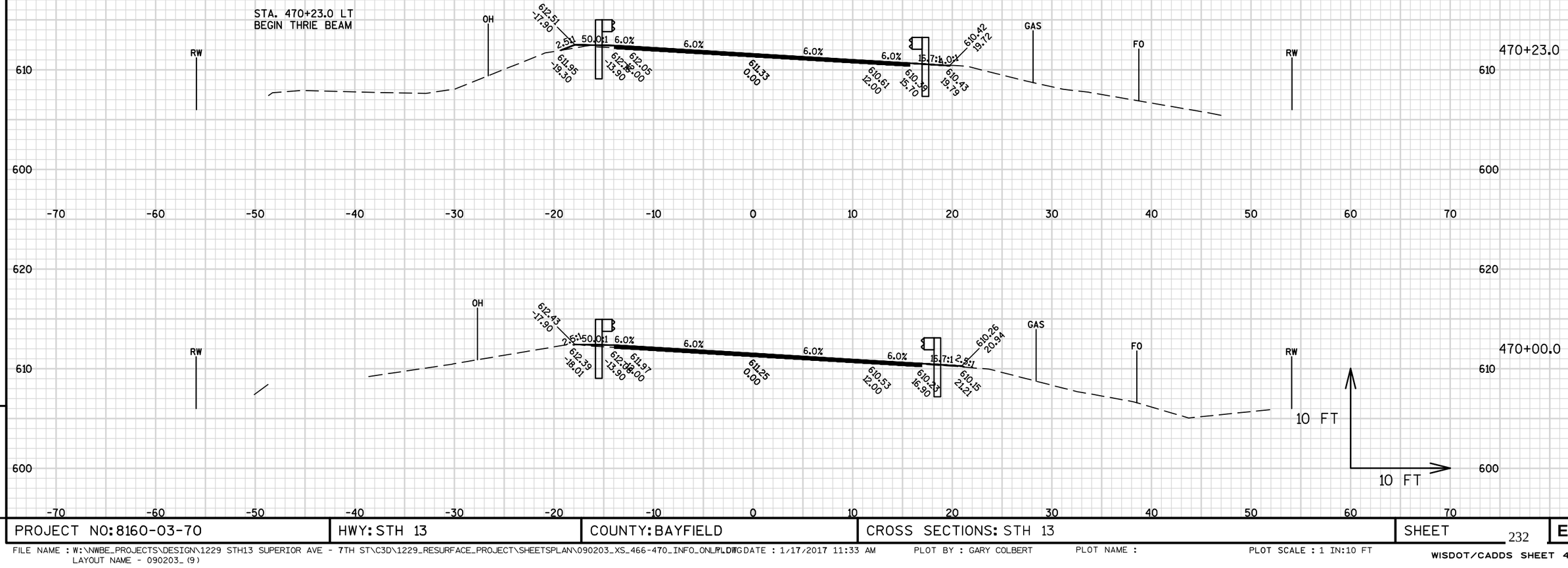
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(8)
PLOTTED DATE : 1/17/2017 11:32 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

STA. 470+48.3 RT
BEGIN THRIE BEAM

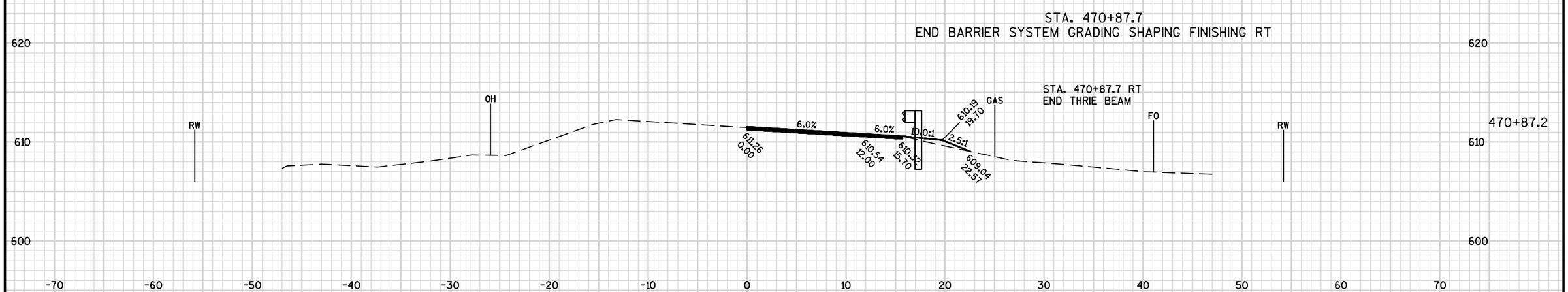


STA. 470+23.0 LT
BEGIN THRIE BEAM



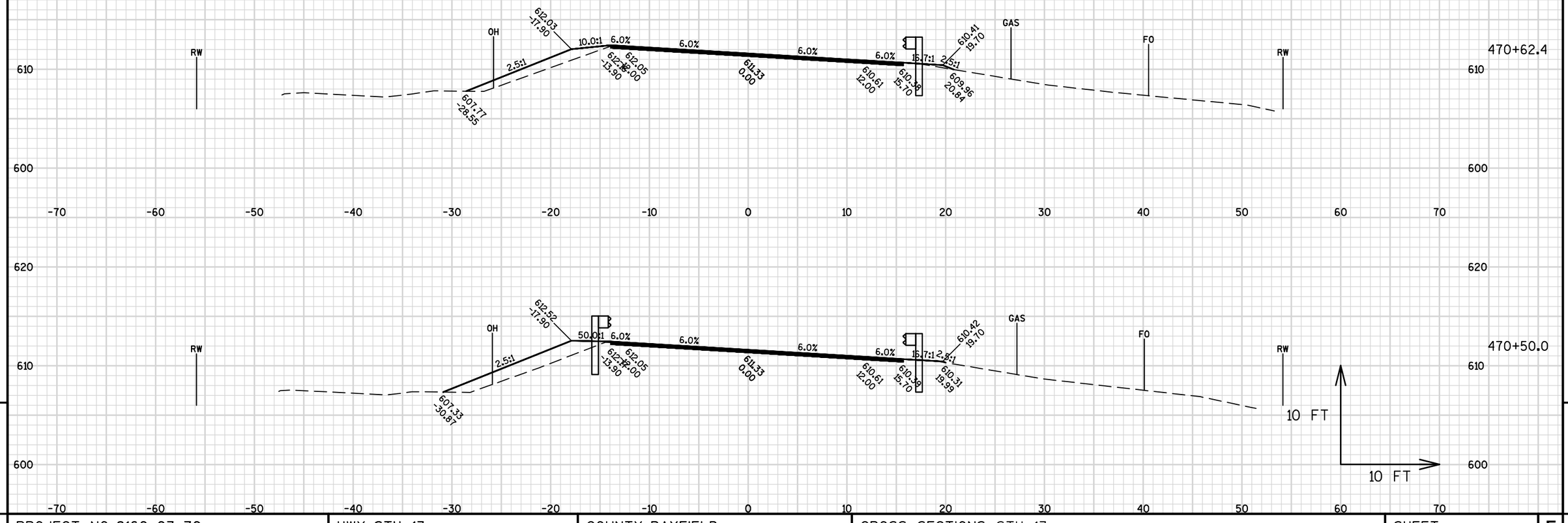
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 232	E

STA. 470+87.7
END BARRIER SYSTEM GRADING SHAPING FINISHING RT



STA. 470+62.4
END BARRIER SYSTEM GRADING SHAPING FINISHING LT

STA. 470+62.4 LT
END THRIE BEAM



9

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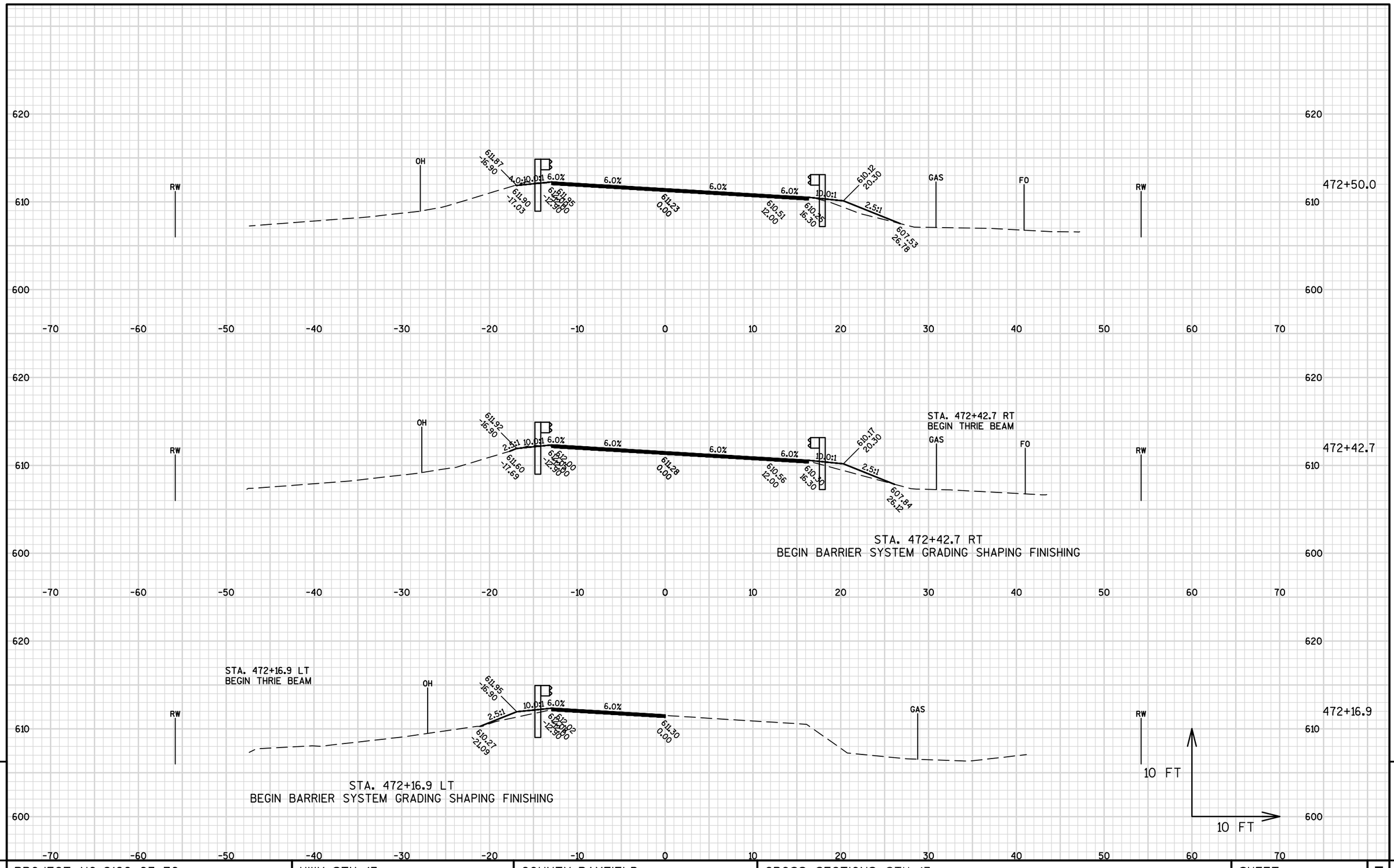
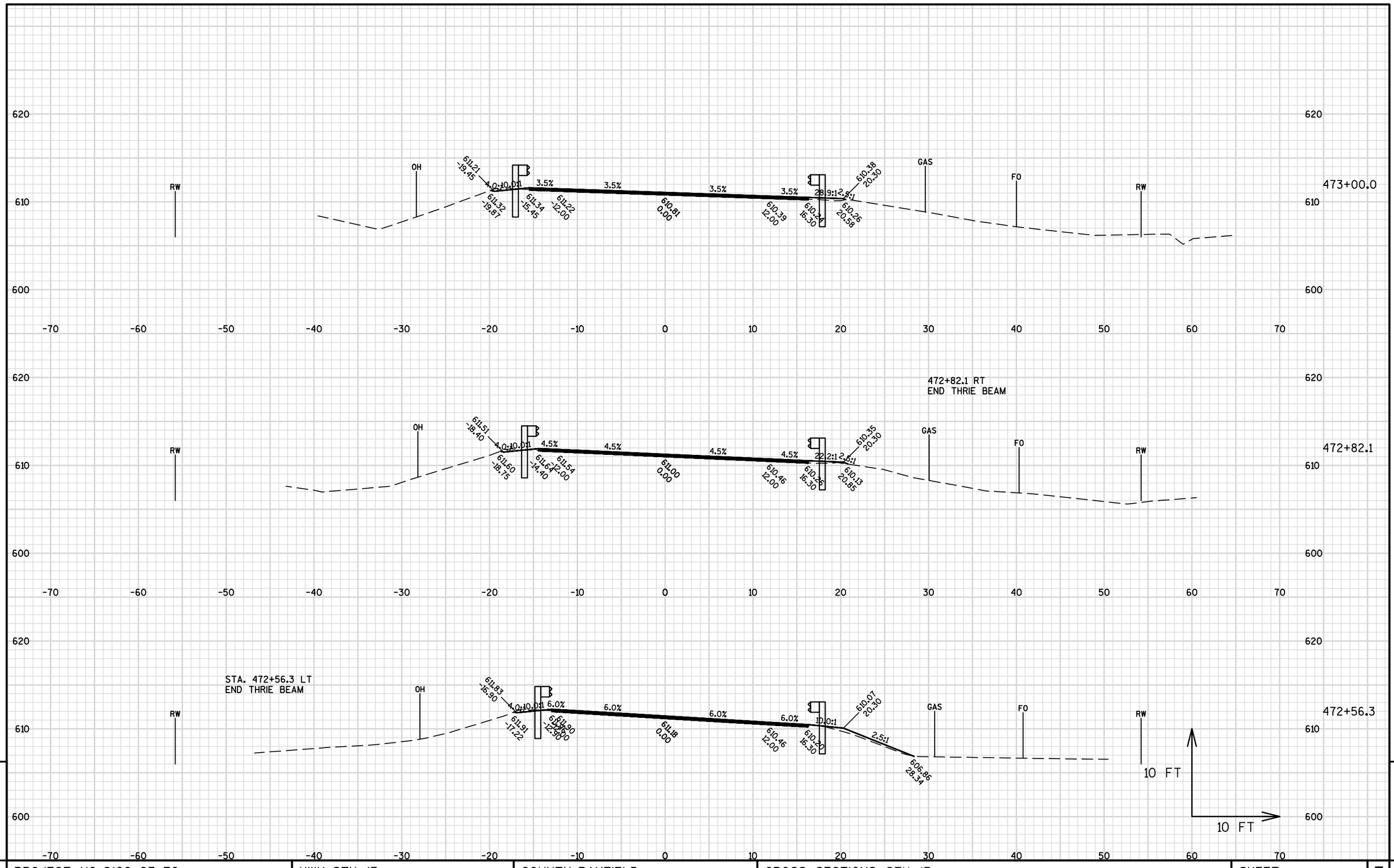
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 233	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(10)
PLOTTED DATE : 1/17/2017 11:33 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 234	E



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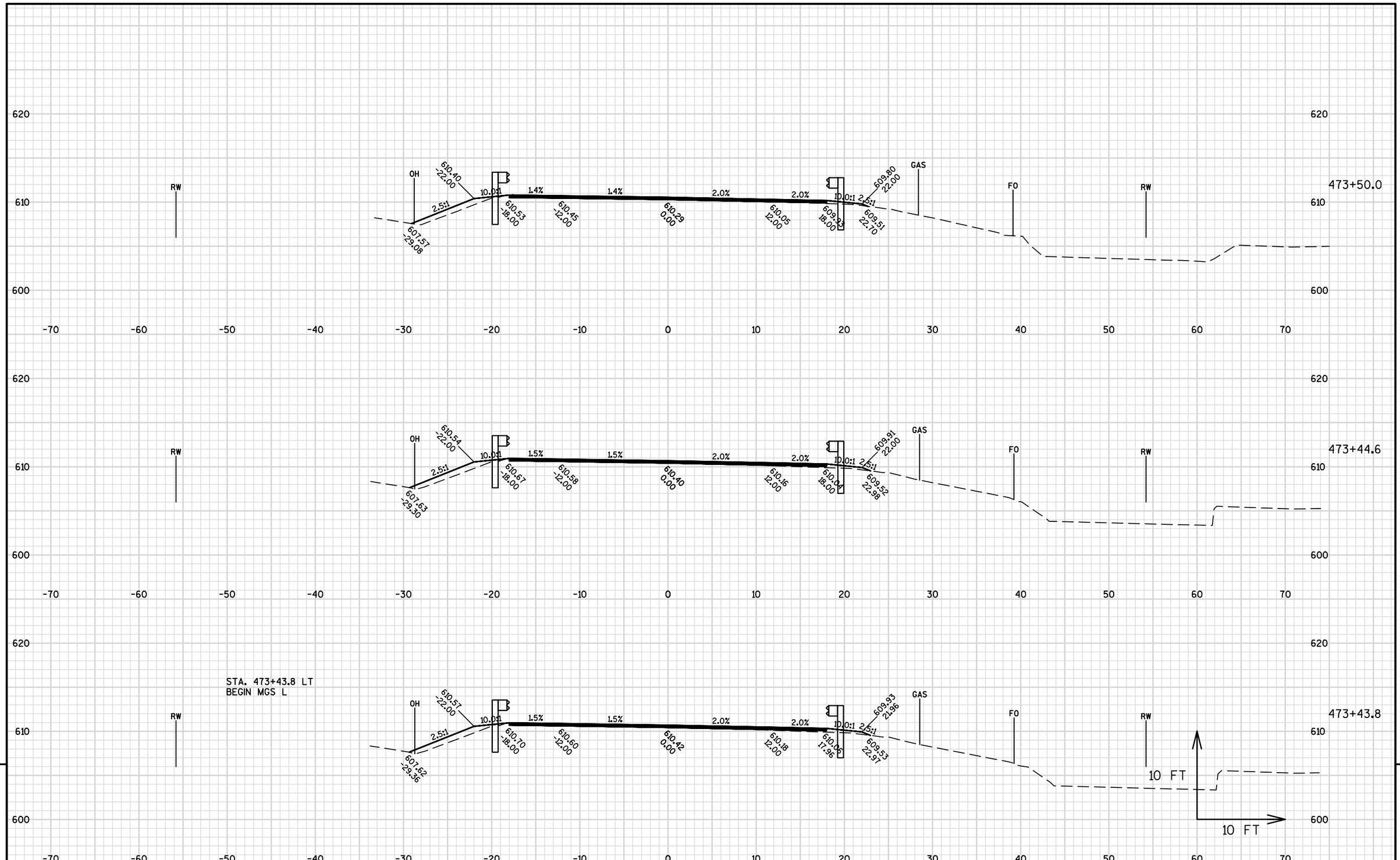
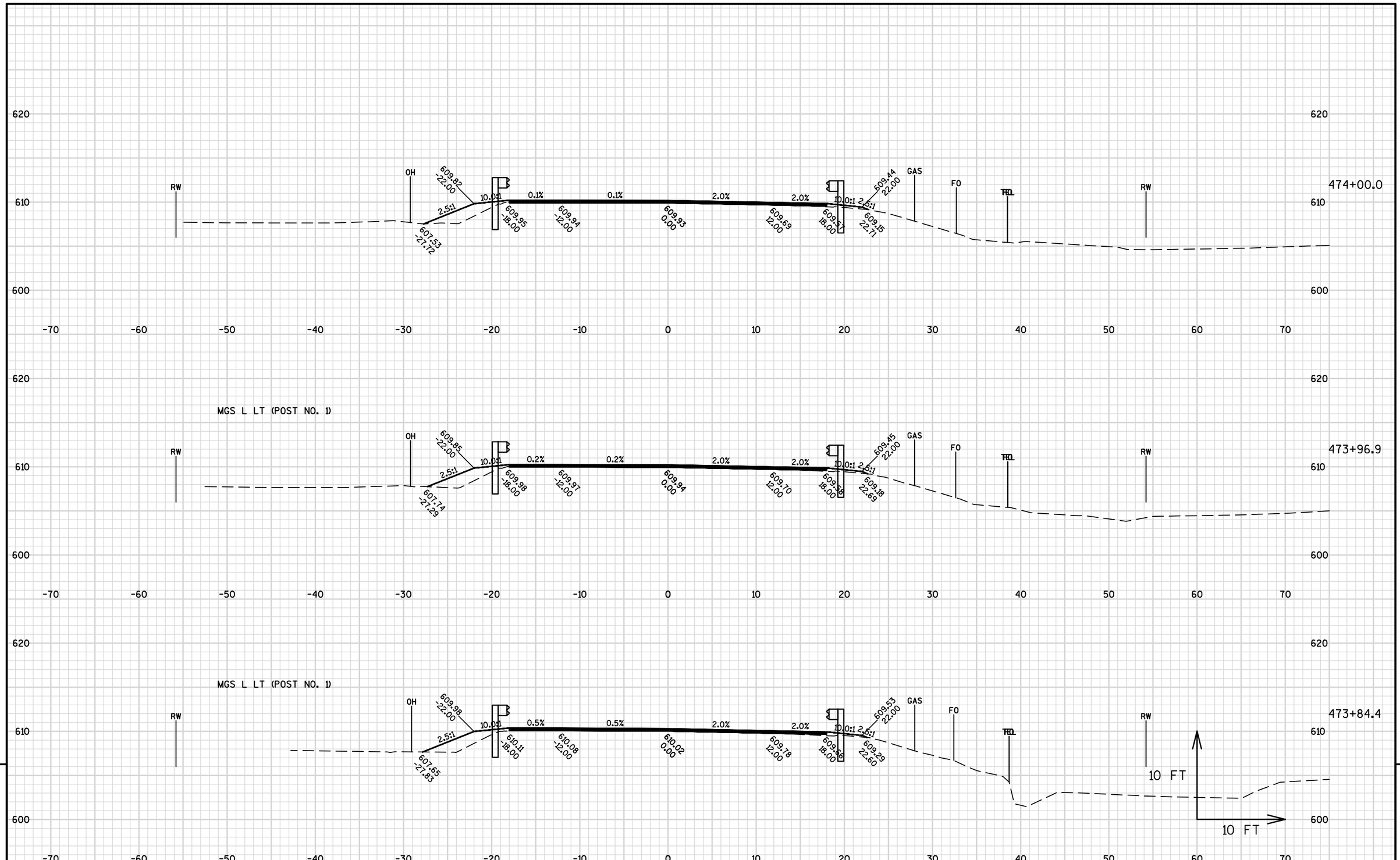
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 235	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG LAYOUT NAME - 090203_(12)
PLOTTED DATE : 1/17/2017 11:33 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

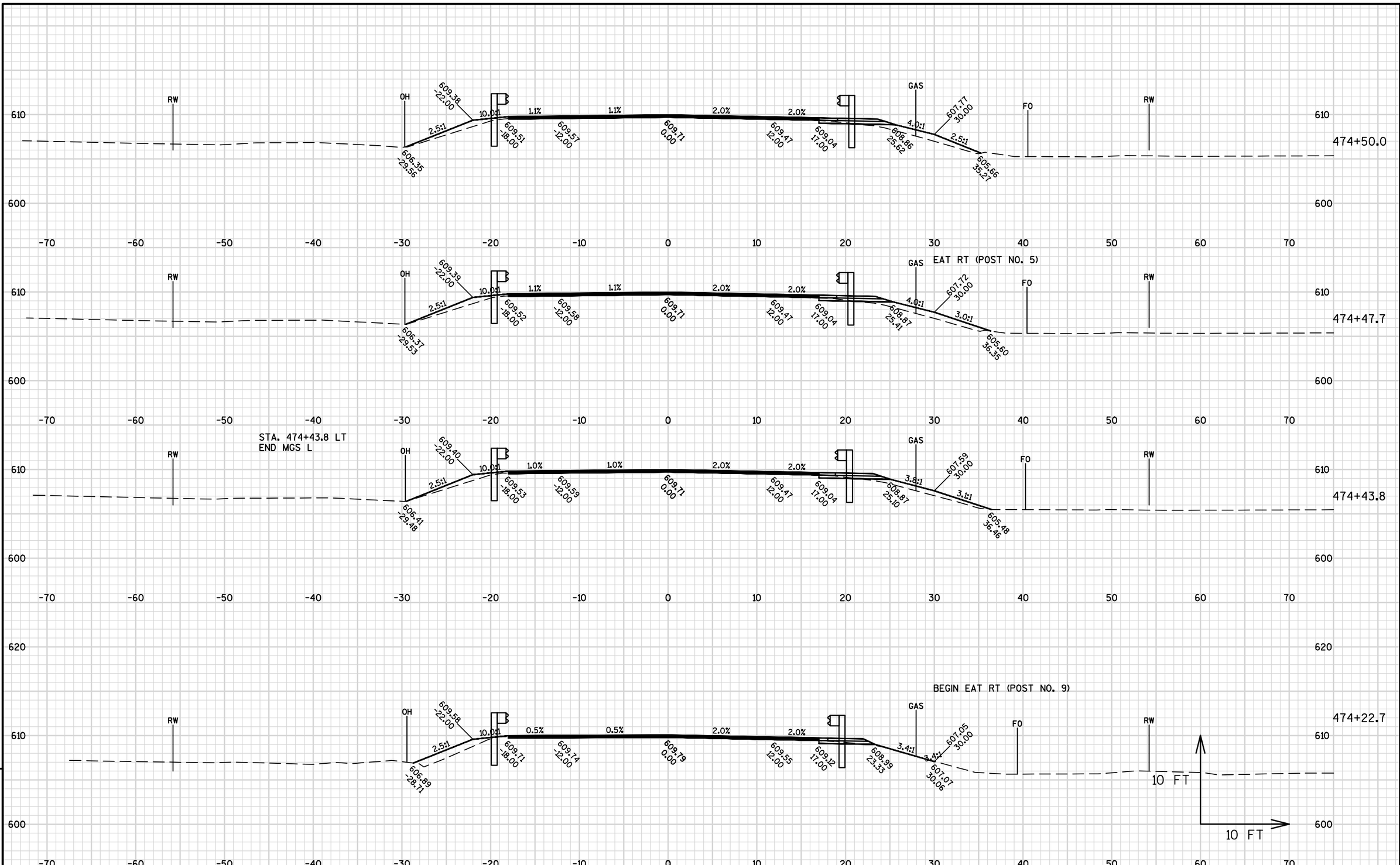


PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 236 E



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 237 E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG LAYOUT NAME - 090203_(14)
PLOTTED : DATE : 1/17/2017 11:34 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49



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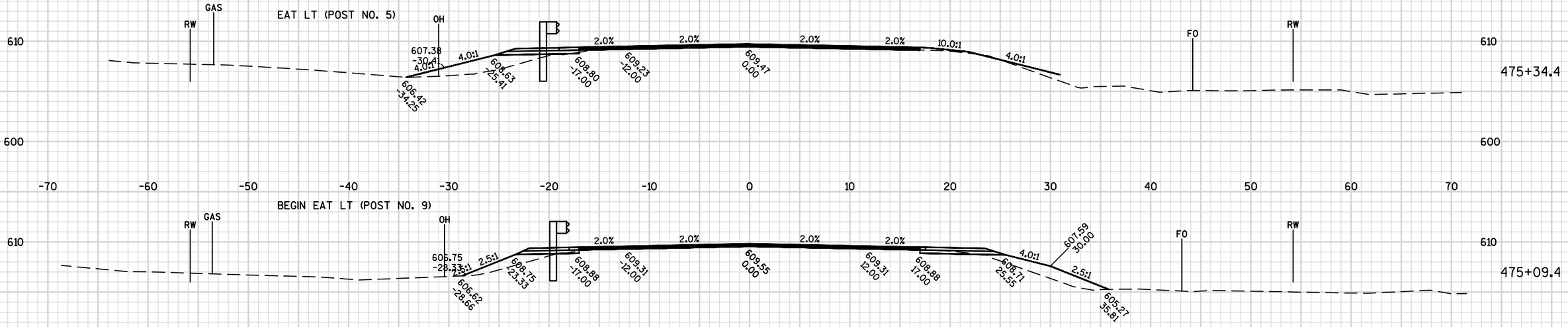
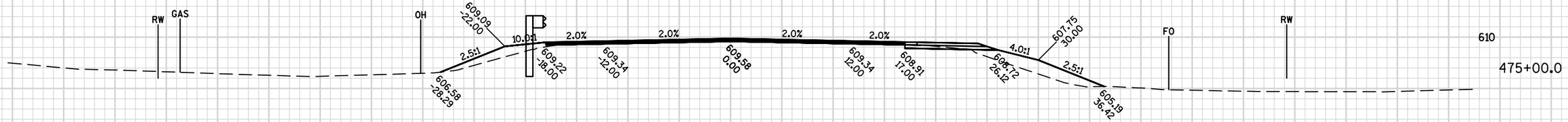
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 238 E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(15)
PLOTTED : 1/17/2017 11:34 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49

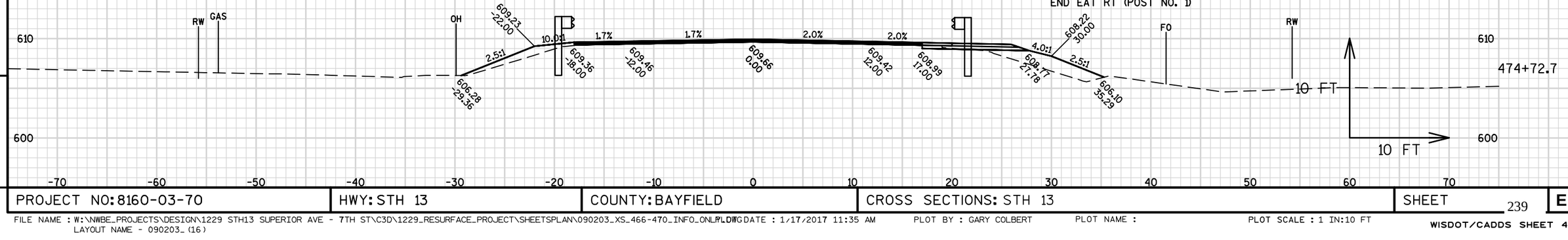
EAT LT (POST NO. 5)



BEGIN EAT LT (POST NO. 9)



END EAT RT (POST NO. 1)



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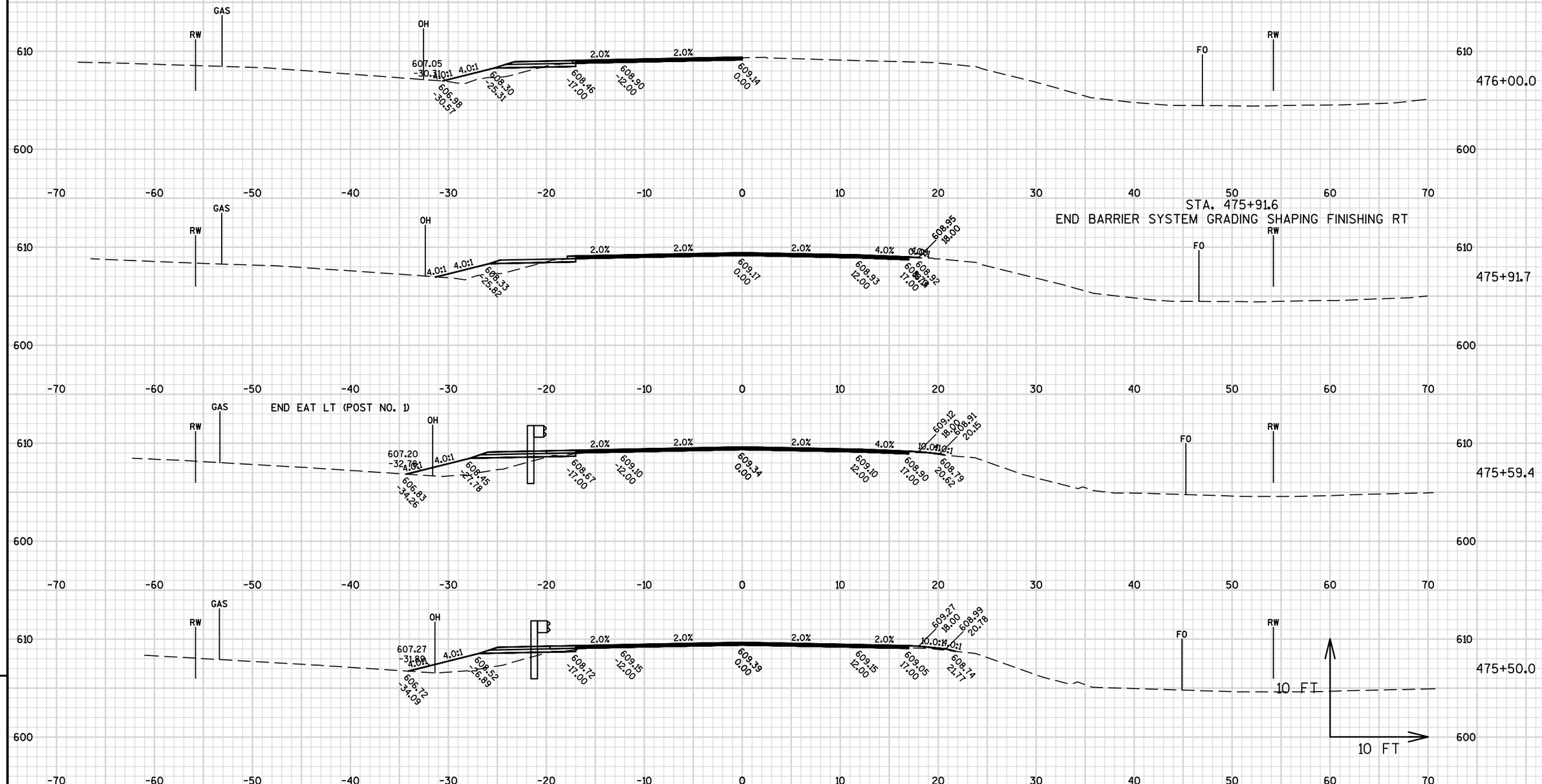
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 239 E



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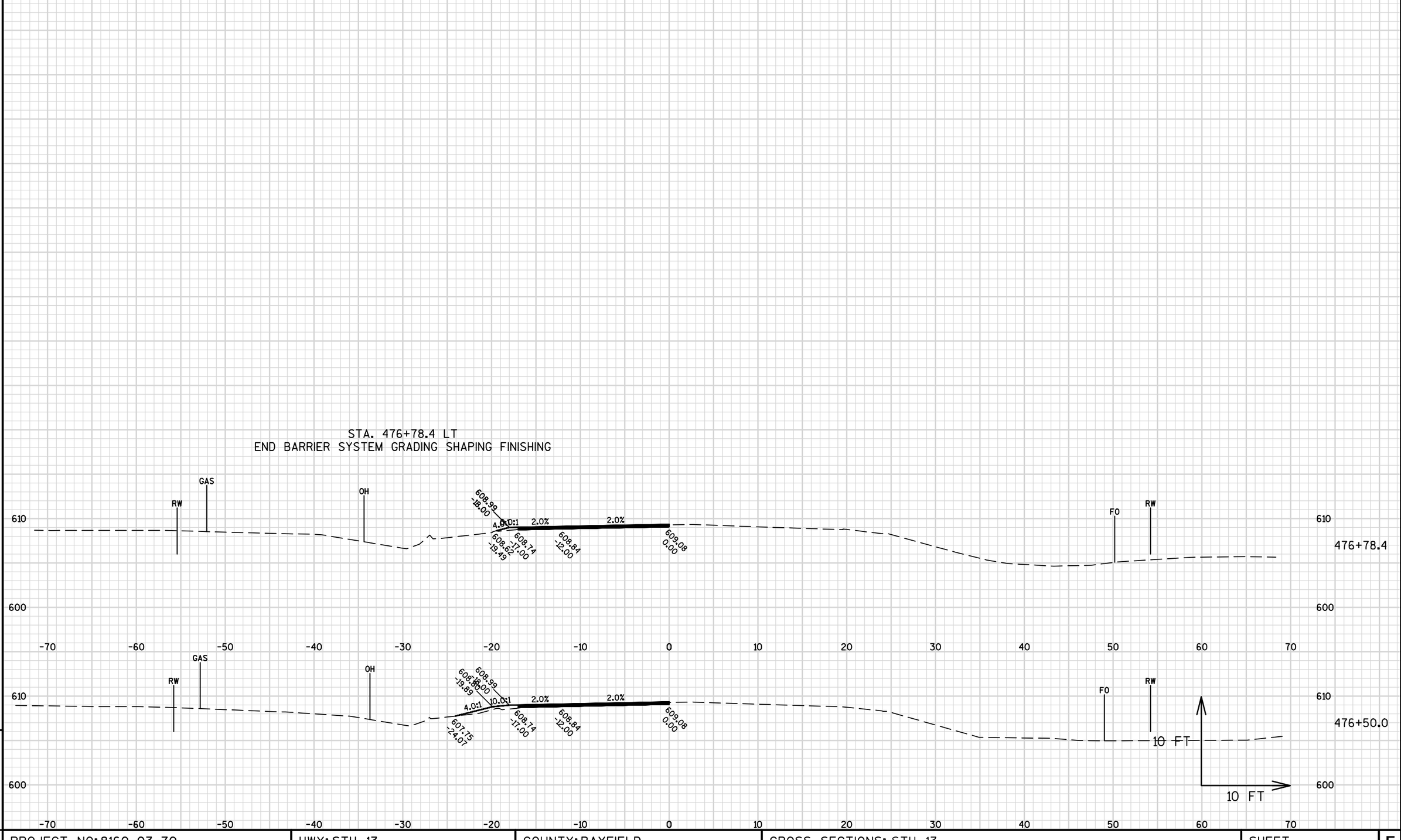
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 240	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(17)
PLOTTED DATE : 1/17/2017 11:35 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 49

STA. 476+78.4 LT
END BARRIER SYSTEM GRADING SHAPING FINISHING



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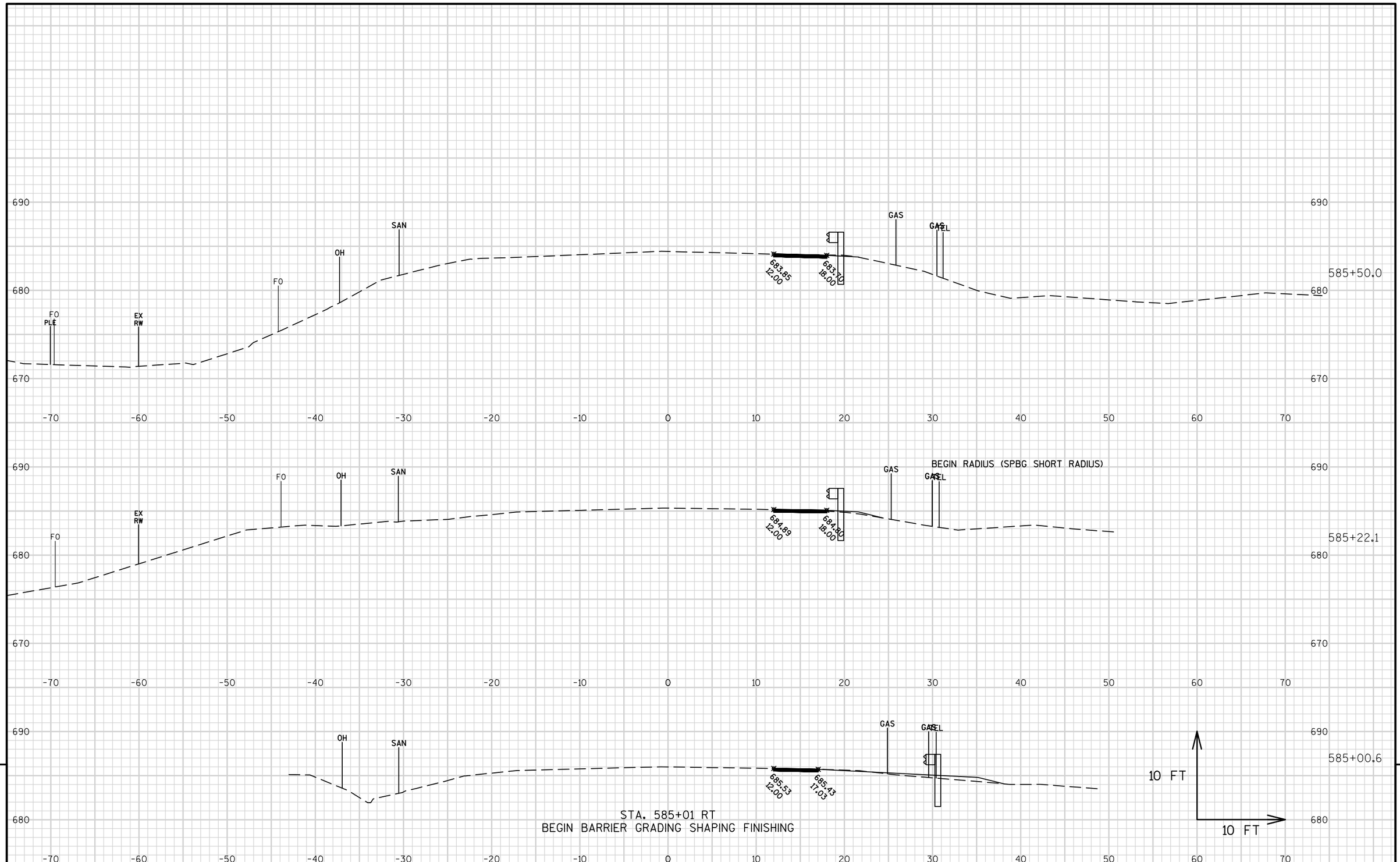
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 241	E

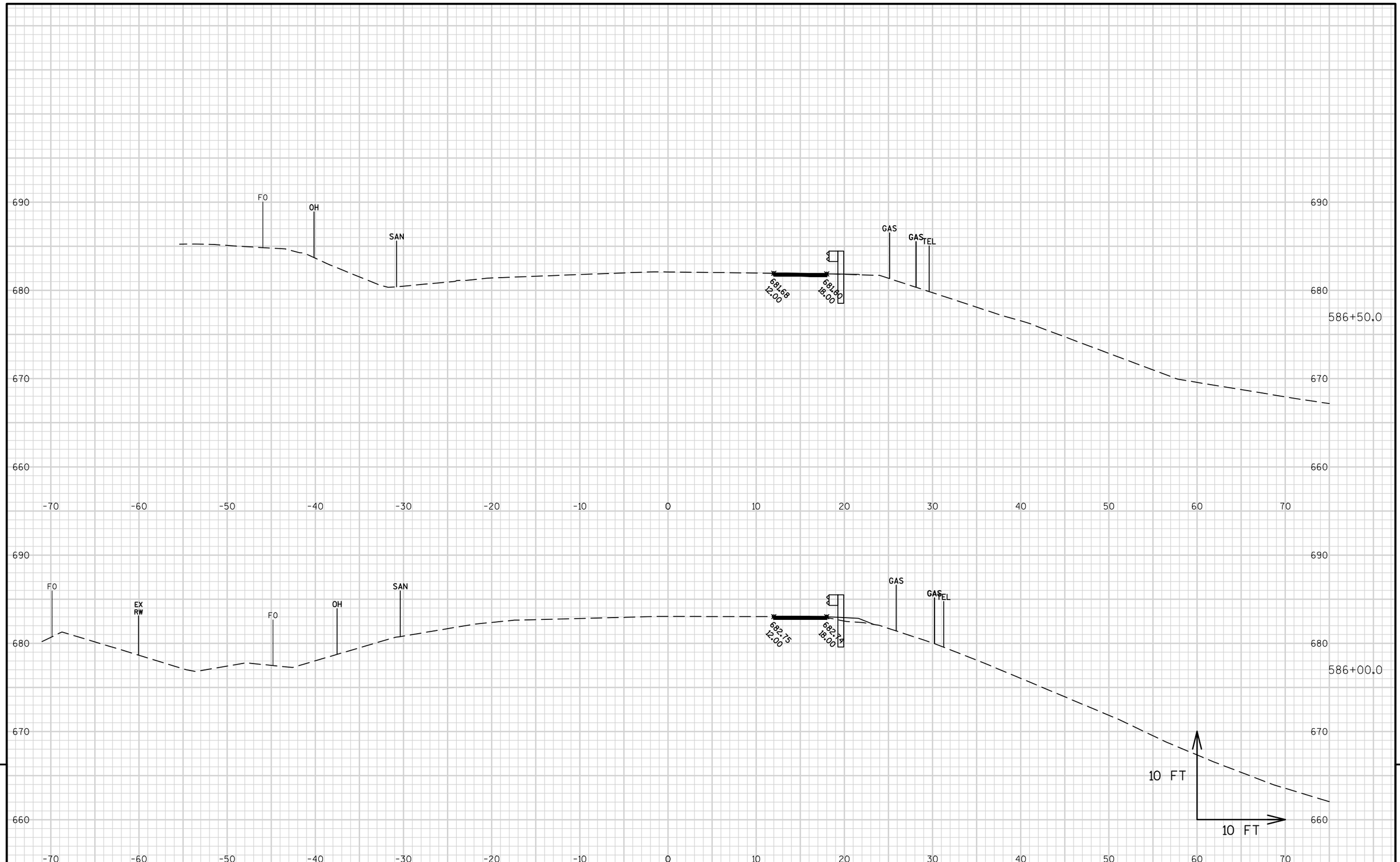
FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090203_XS_466-470_INFO_ONLY.DWG
LAYOUT NAME - 090203_(17)
PLOTTED DATE : 1/17/2017 11:35 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG 585+01-588+77 RT-INFO. ONLY	SHEET 242	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\XS_585-589_INFOONLY.DWG PLOT DATE : 1/11/2017 4:52 PM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-585-589-RT1

WISDOT/CADDS SHEET 49



9

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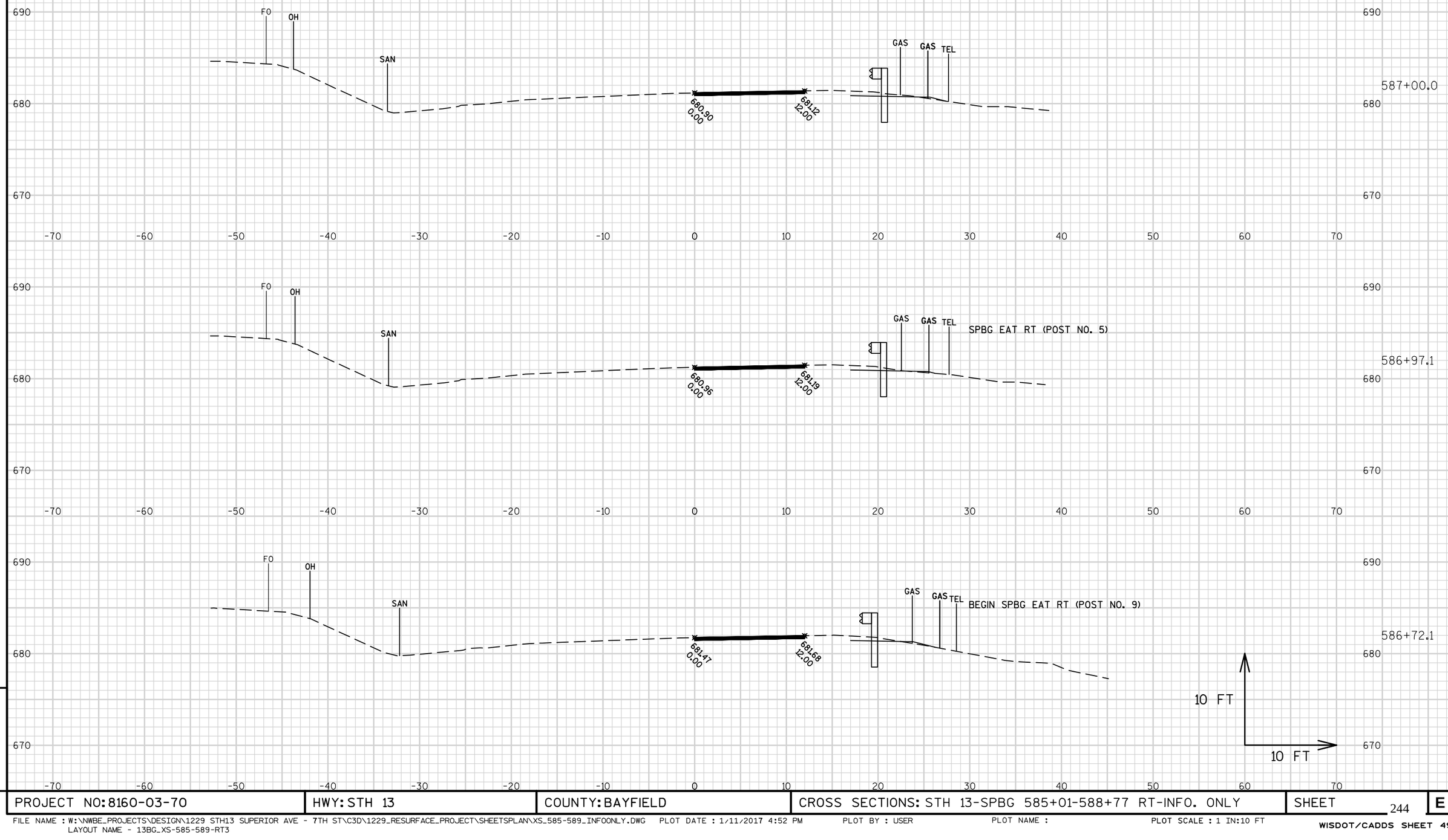
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG 585+01-588+77 RT-INFO. ONLY	SHEET 243	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\XS_585-589_INFOONLY.DWG PLOT DATE : 1/11/2017 4:52 PM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-585-589-RT2

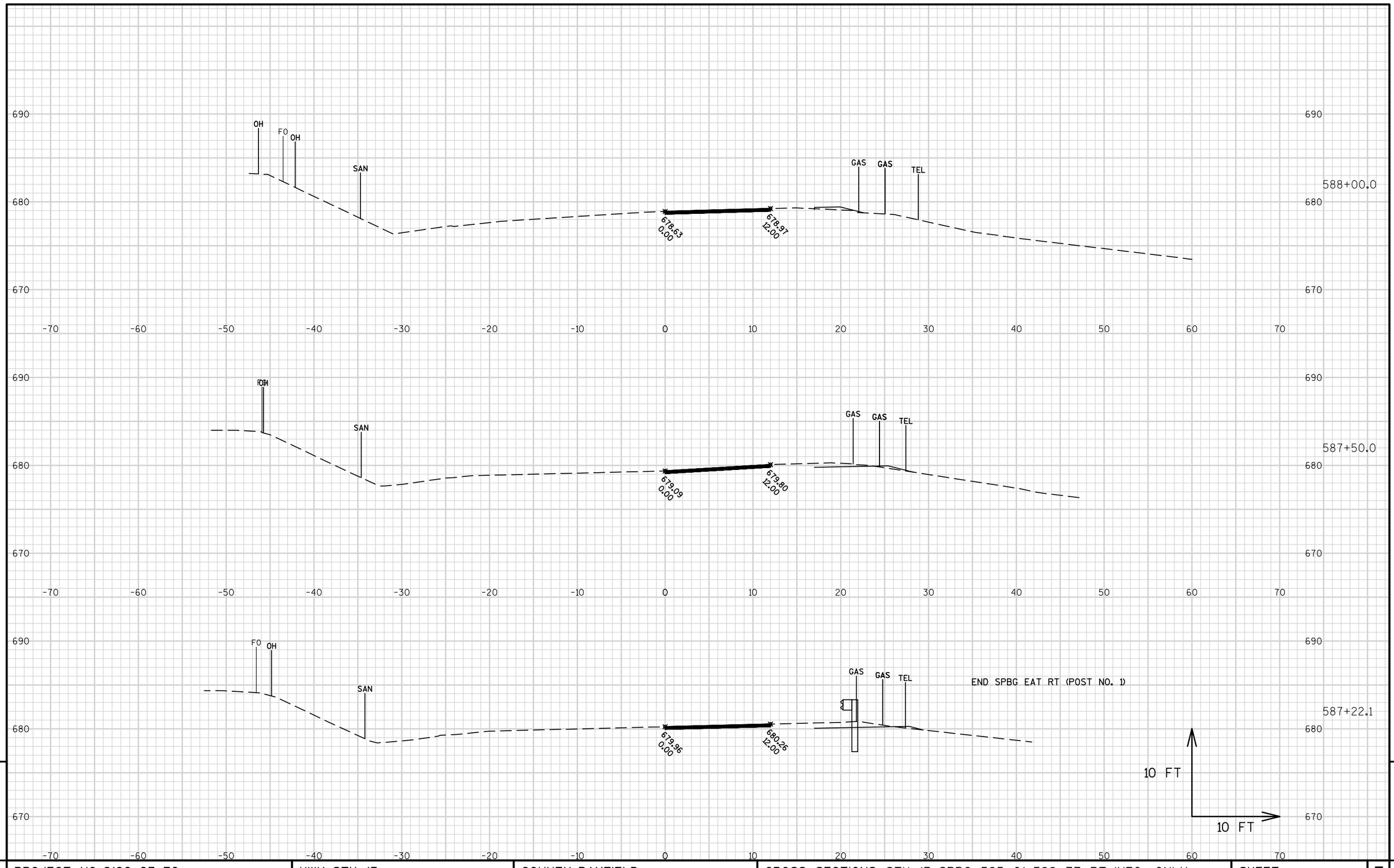


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG 585+01-588+77 RT-INFO. ONLY	SHEET 244	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\XS_585-589_INFOONLY.DWG PLOT DATE : 1/11/2017 4:52 PM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-585-589-RT3



9

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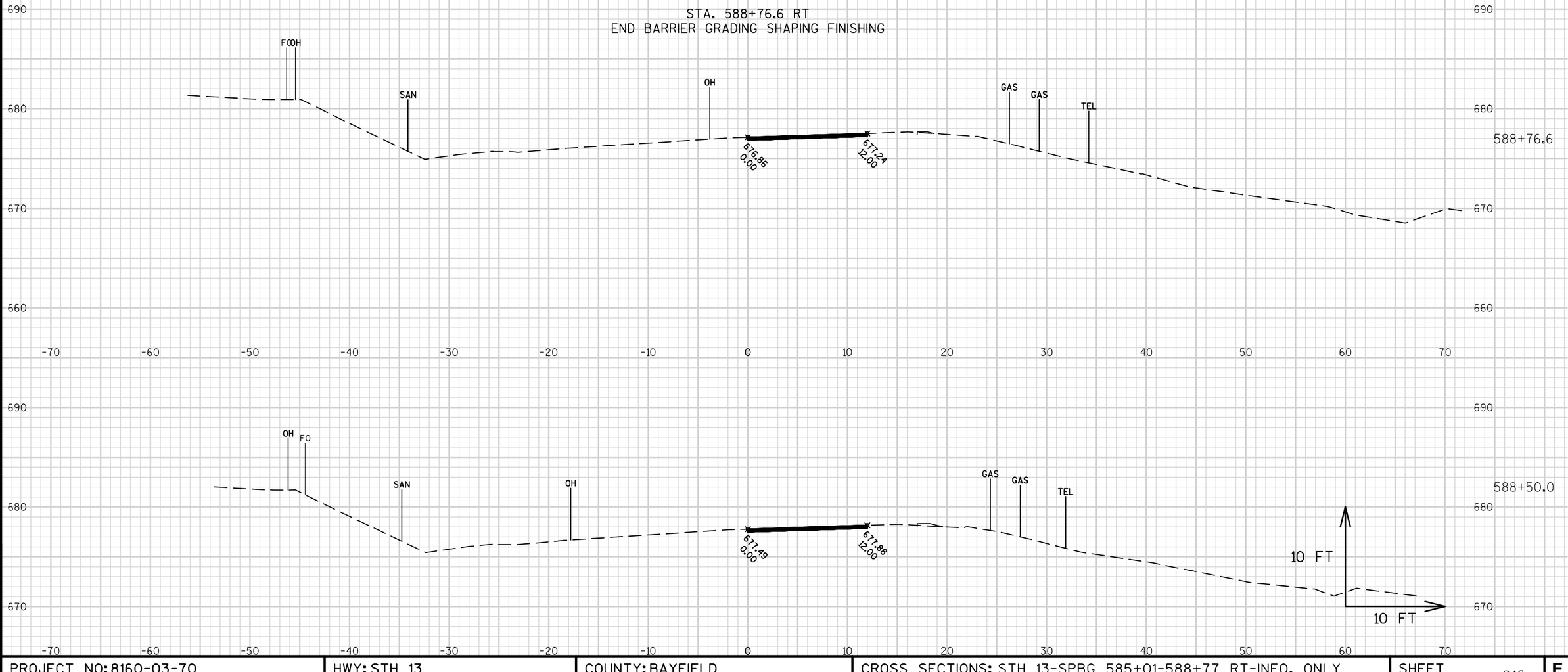
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG 585+01-588+77 RT-INFO. ONLY	SHEET 245	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\XS_585-589_INFOONLY.DWG PLOT DATE : 1/11/2017 4:52 PM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-585-589-RT4

STA. 588+76.6 RT
END BARRIER GRADING SHAPING FINISHING

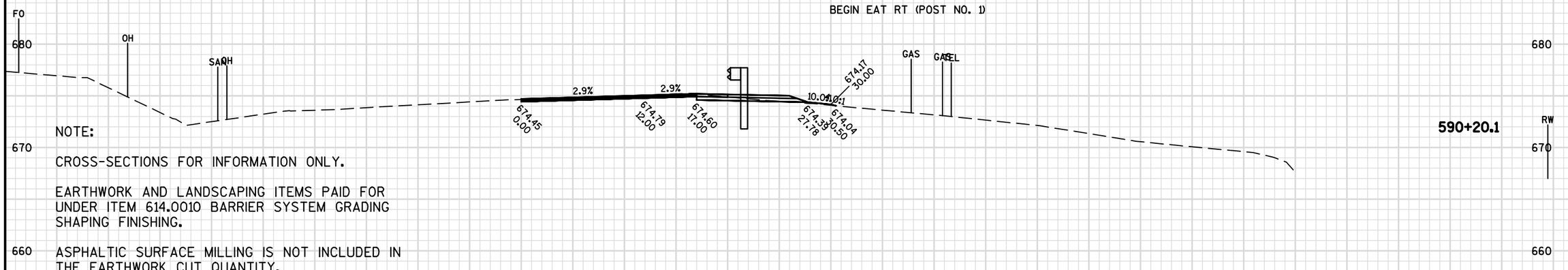


PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13-SPBG 585+01-588+77 RT-INFO. ONLY	SHEET 246	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\XS_585-589_INFOONLY.DWG PLOT DATE : 1/11/2017 4:52 PM PLOT BY : USER PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 13BG_XS-585-589-RT5

WISDOT/CADDS SHEET 49

BEGIN EAT RT (POST NO. 1)

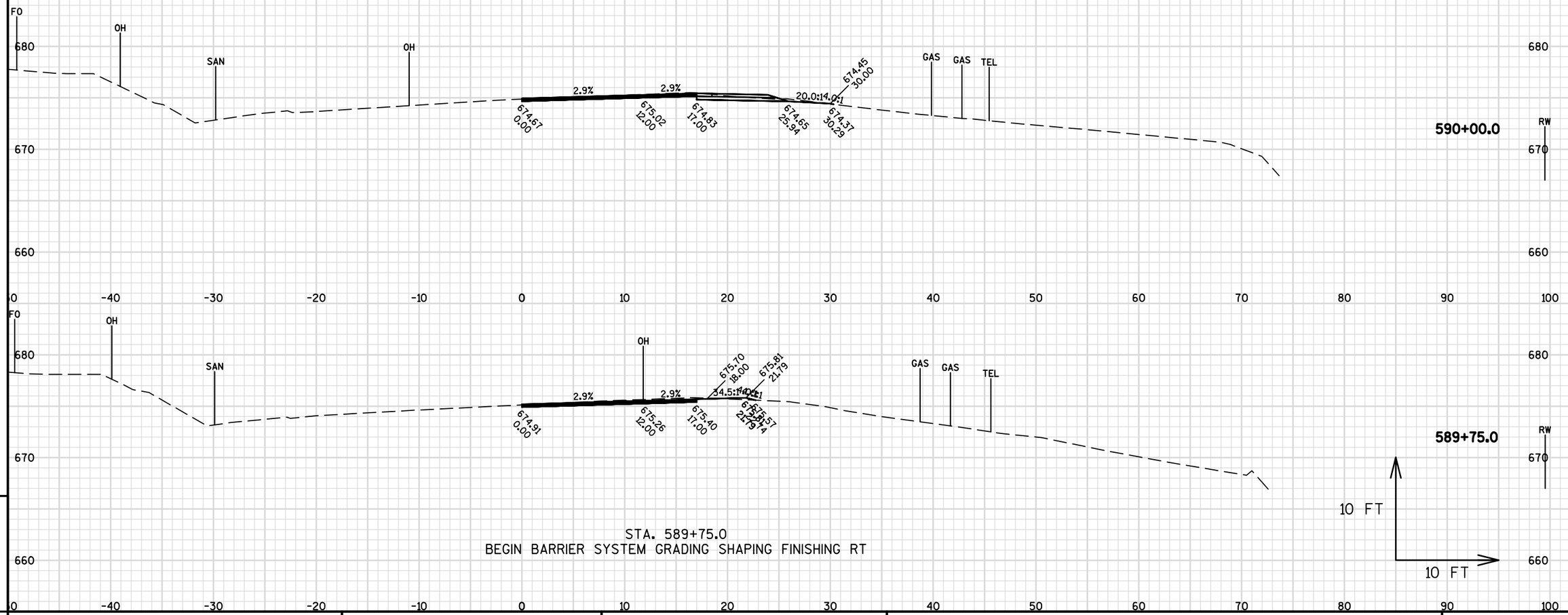


NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.



STA. 589+75.0
BEGIN BARRIER SYSTEM GRADING SHAPING FINISHING RT

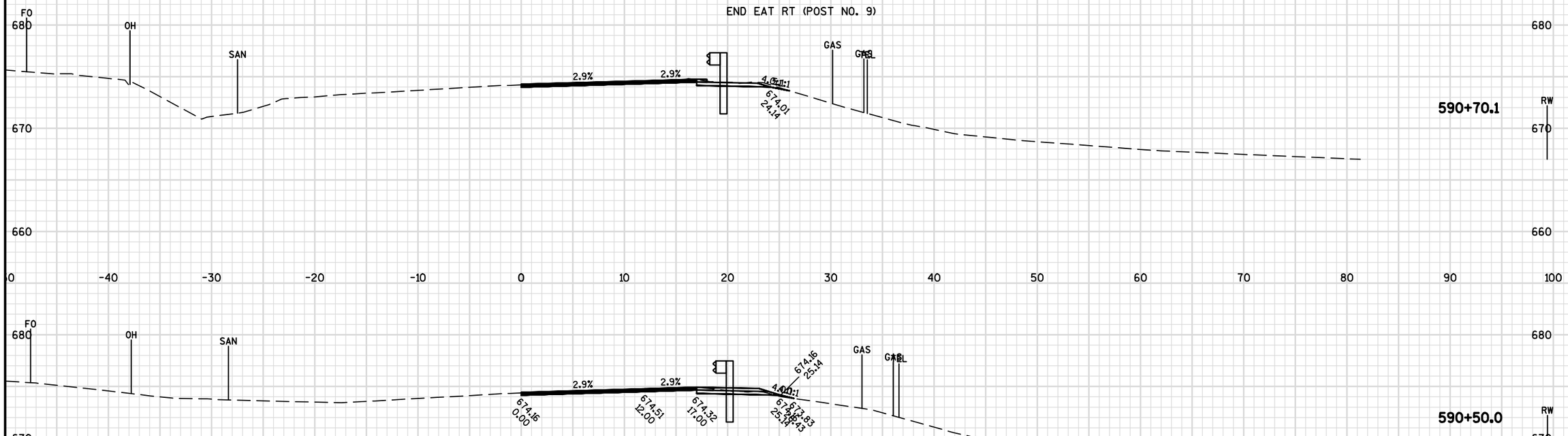
9 9

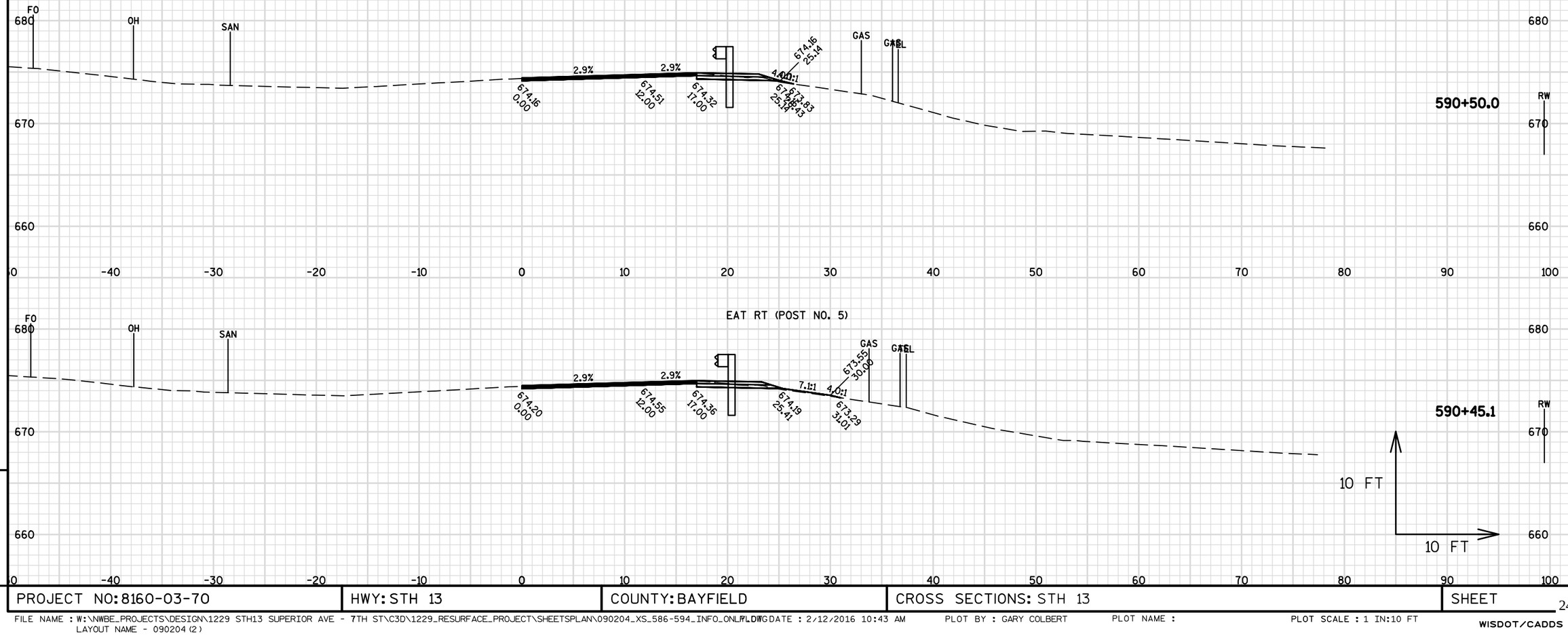
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 247 E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090204_XS_586-594_INFO_ONLY.DWG PLOTTING DATE : 2/12/2016 10:43 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090204 (1)

WISDOT/CADDS SHEET 49

END EAT RT (POST NO. 9)





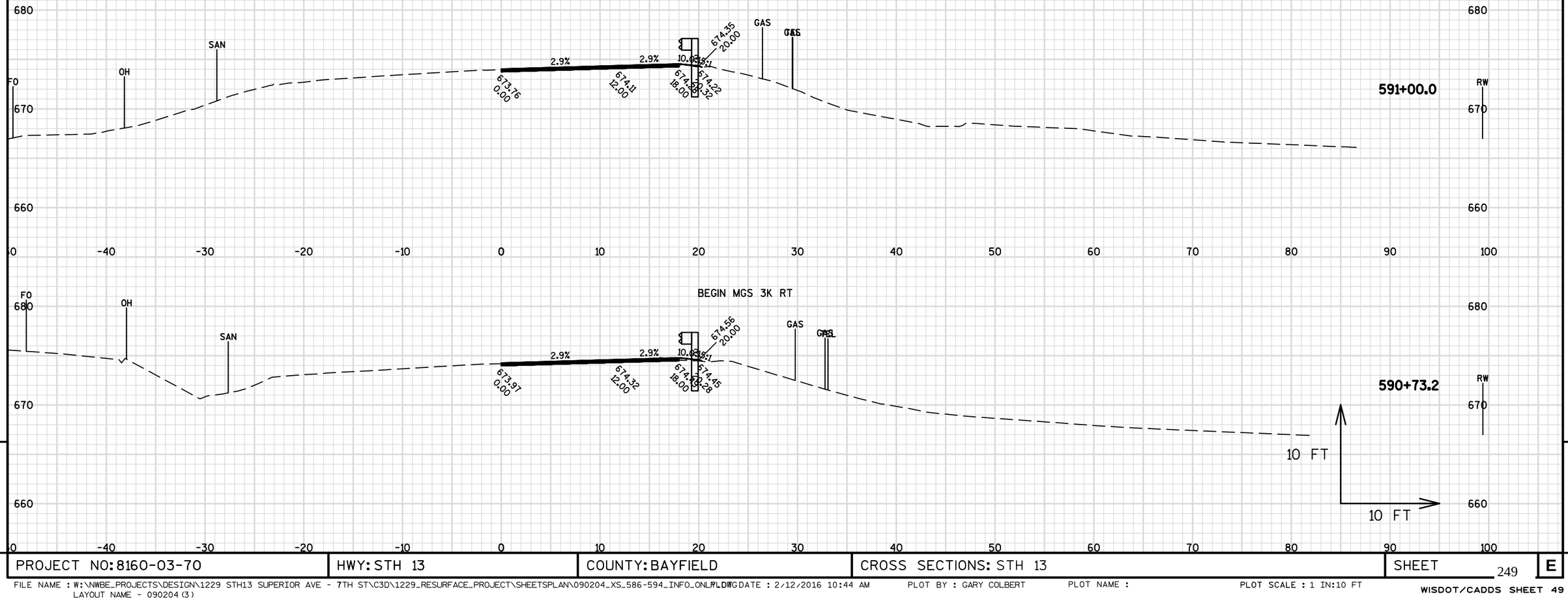
EAT RT (POST NO. 5)

9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090204_XS_586-594_INFO_ONLY.DWG DATE : 2/12/2016 10:43 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090204 (2)

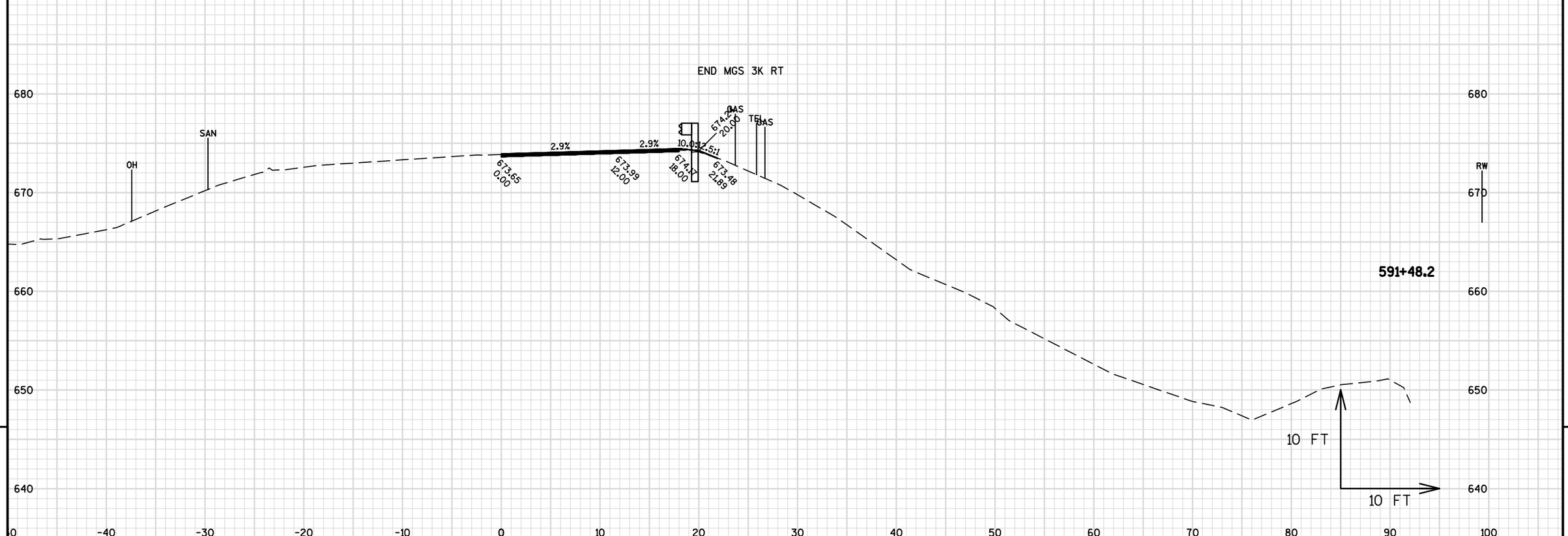


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 249	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090204_XS_586-594_INFO_ONLY.DWG PLOTTED DATE : 2/12/2016 10:44 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090204 (3)

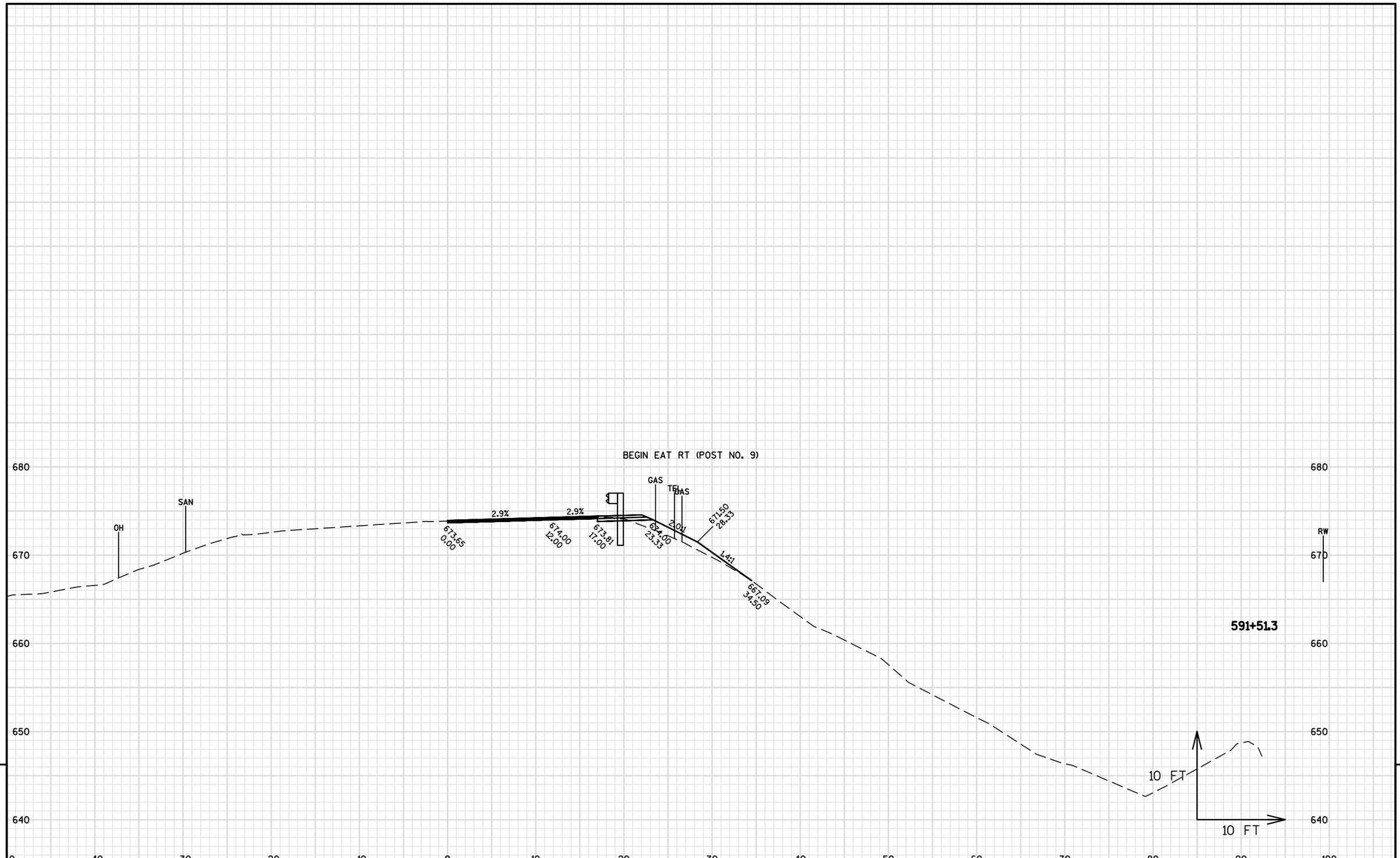


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 250	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090204_XS_586-594_INFO_ONLY.DWG
LAYOUT NAME - 090204 (4)
PLOTTED DATE : 2/12/2016 10:44 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



9

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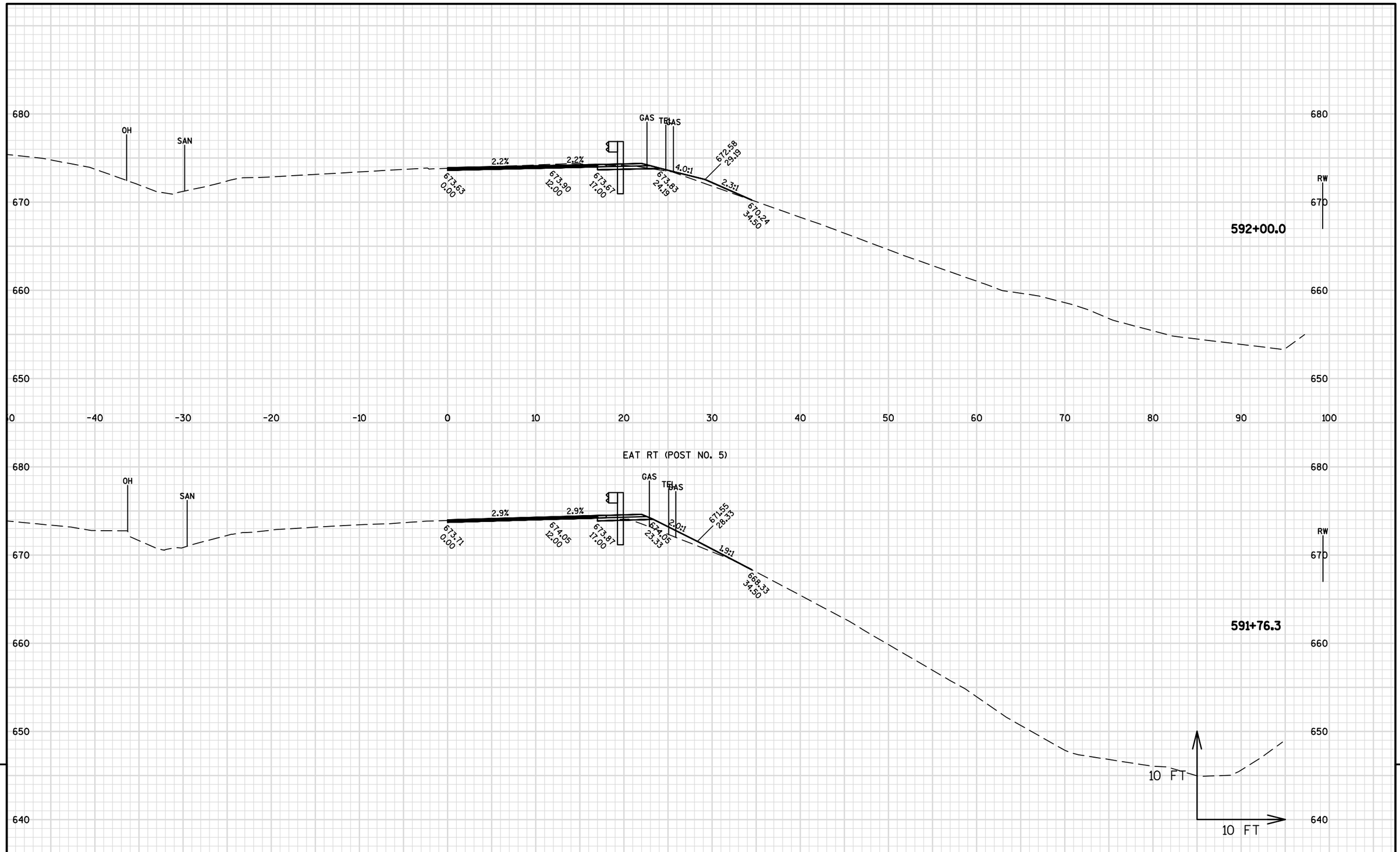
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 251	E

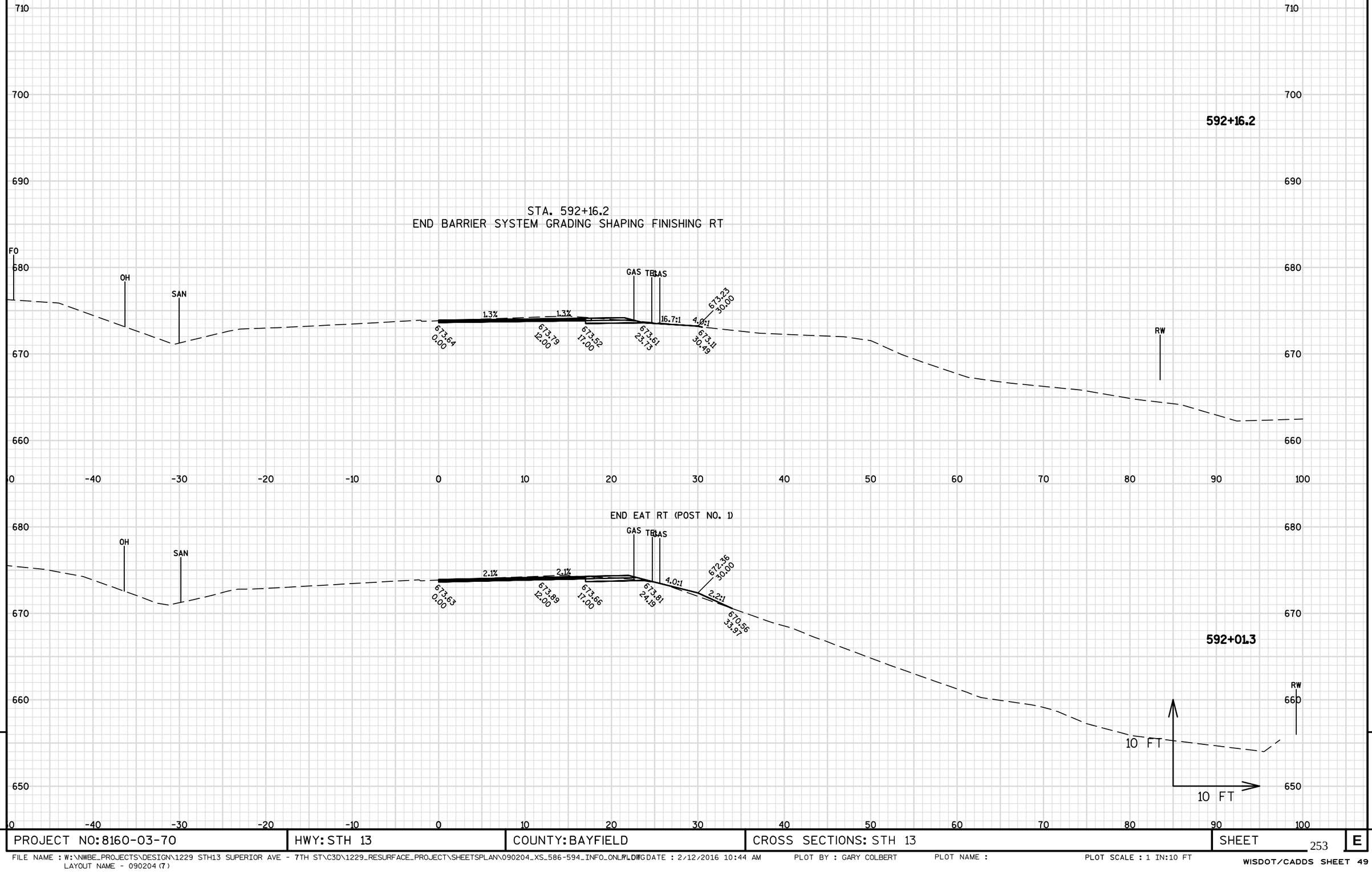


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 252	E

STA. 592+16.2
END BARRIER SYSTEM GRADING SHAPING FINISHING RT



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 253	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090204_XS_586-594_INFO_ONLY.DWG
LAYOUT NAME - 090204 (7)
PLOTTED DATE : 2/12/2016 10:44 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

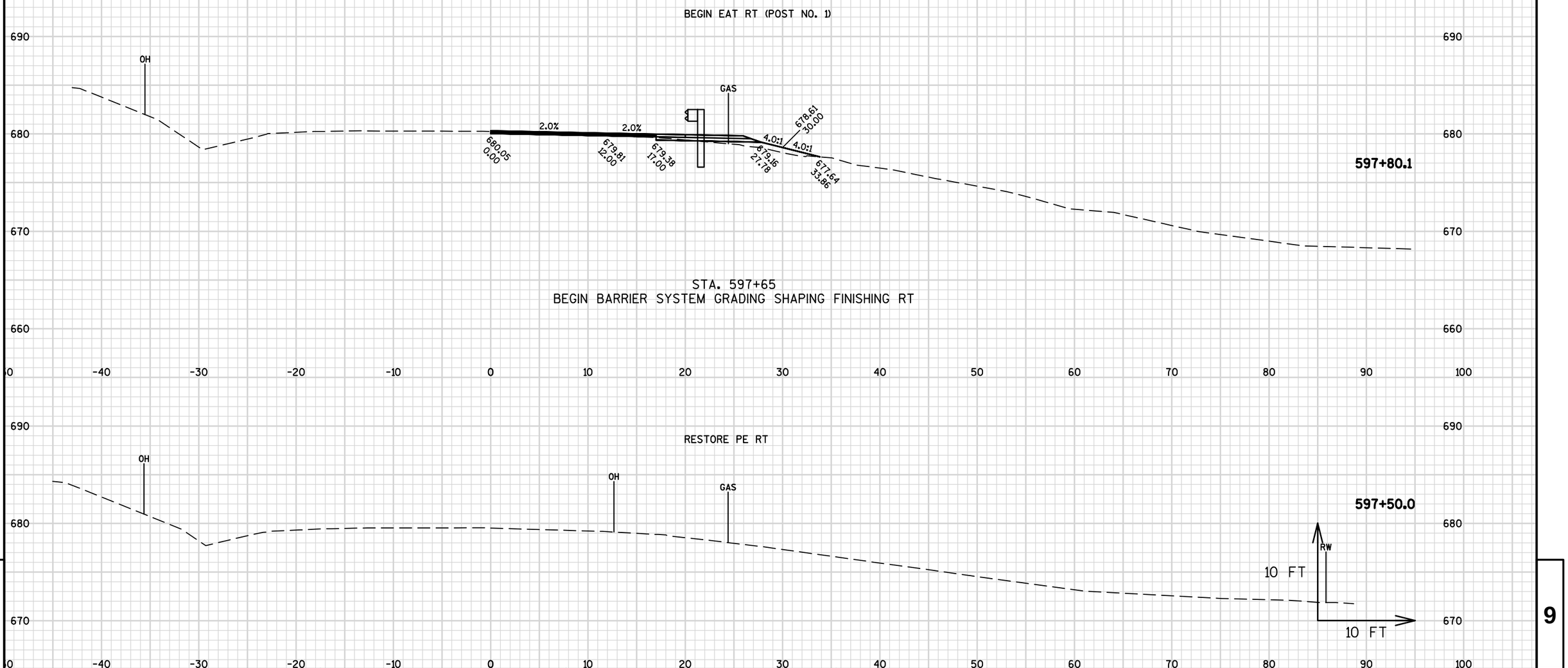
WISDOT/CADDS SHEET 49

NOTE:

CROSS-SECTIONS FOR INFORMATION ONLY.

EARTHWORK AND LANDSCAPING ITEMS PAID FOR UNDER ITEM 614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING.

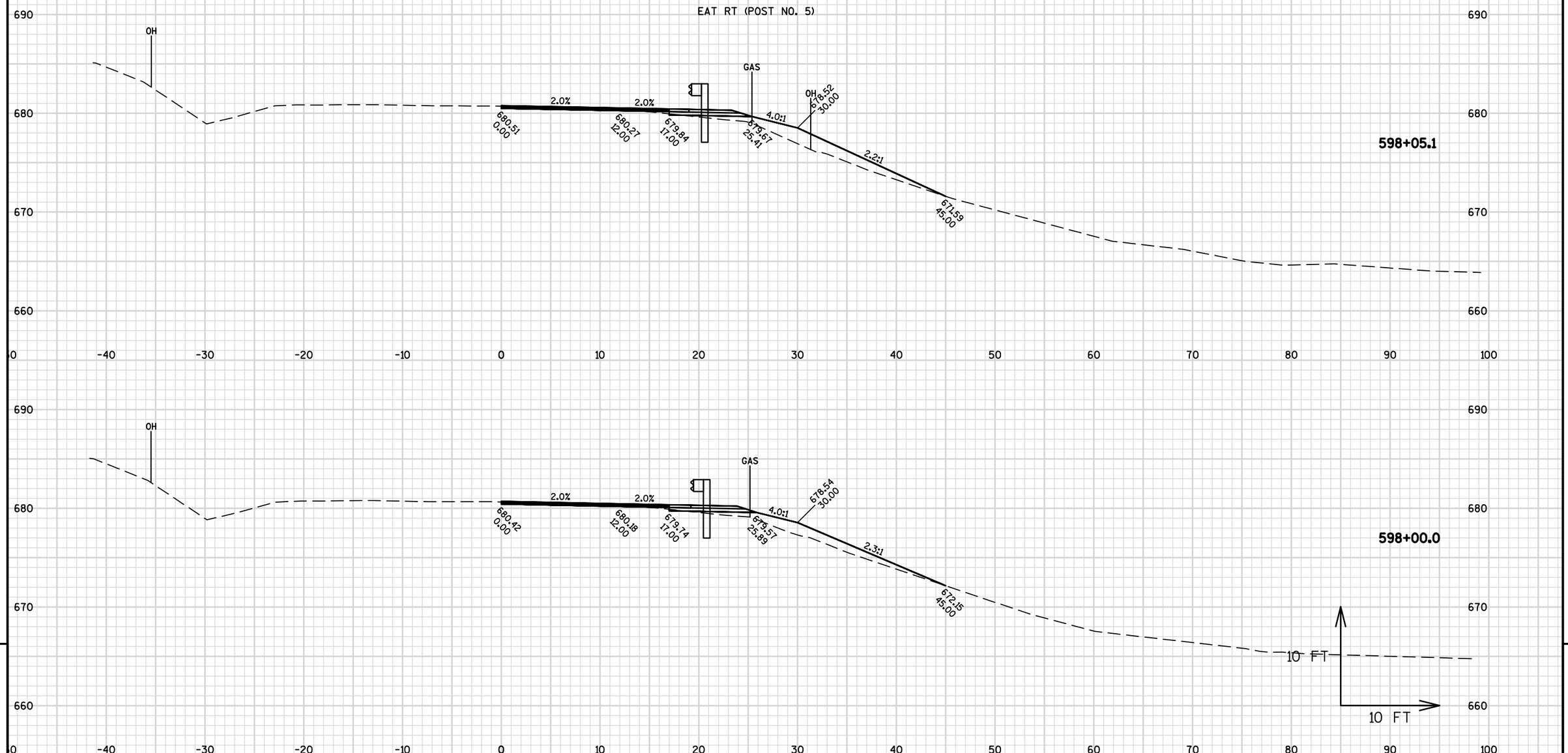
ASPHALTIC SURFACE MILLING IS NOT INCLUDED IN THE EARTHWORK CUT QUANTITY.



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 254	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG DATE : 1/17/2017 11:45 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090205 (2)

WISDOT/CADDS SHEET 49



9

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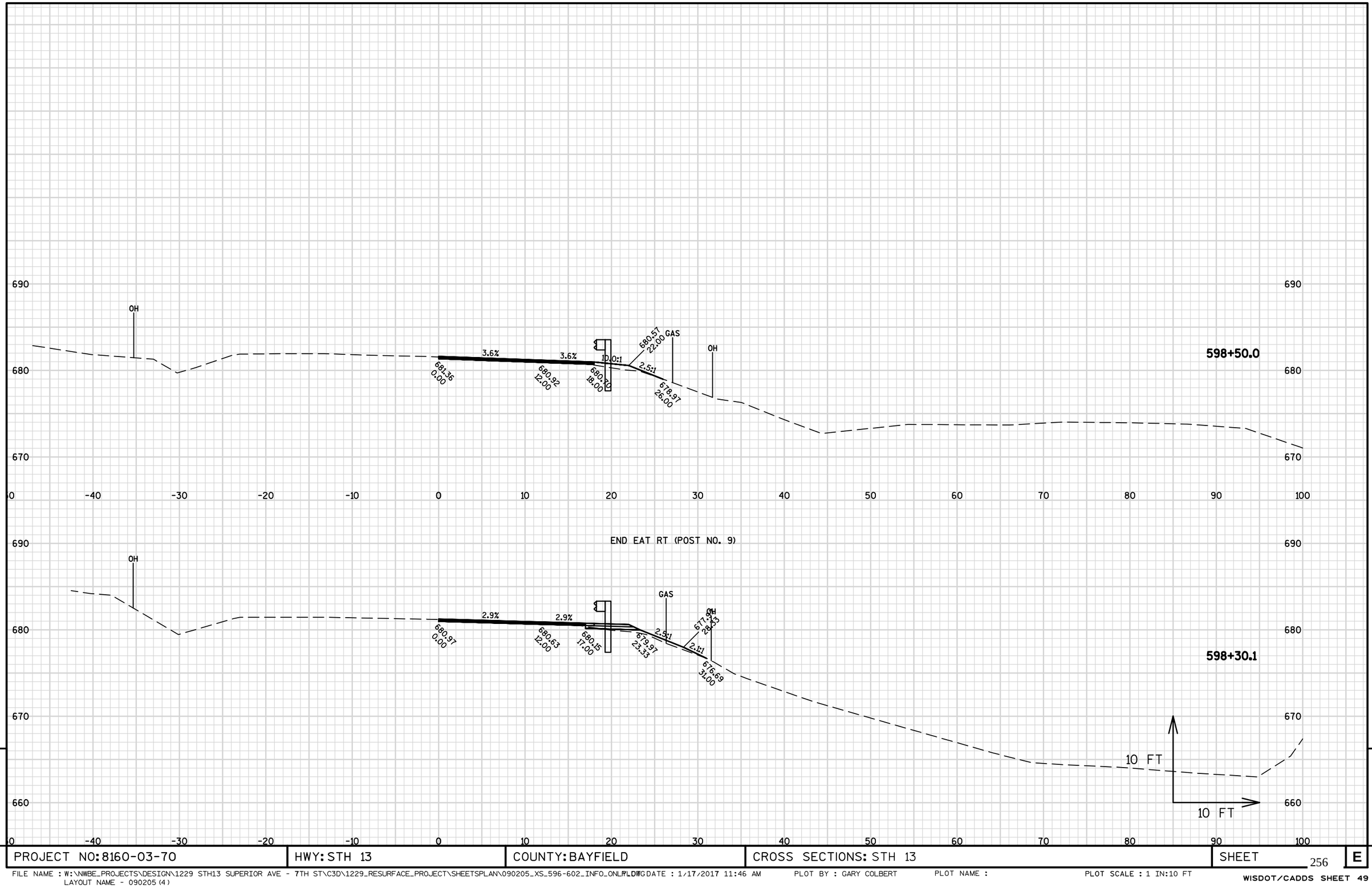
PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 255	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG
LAYOUT NAME - 090205 (3)
PLOTTED DATE : 1/17/2017 11:45 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49



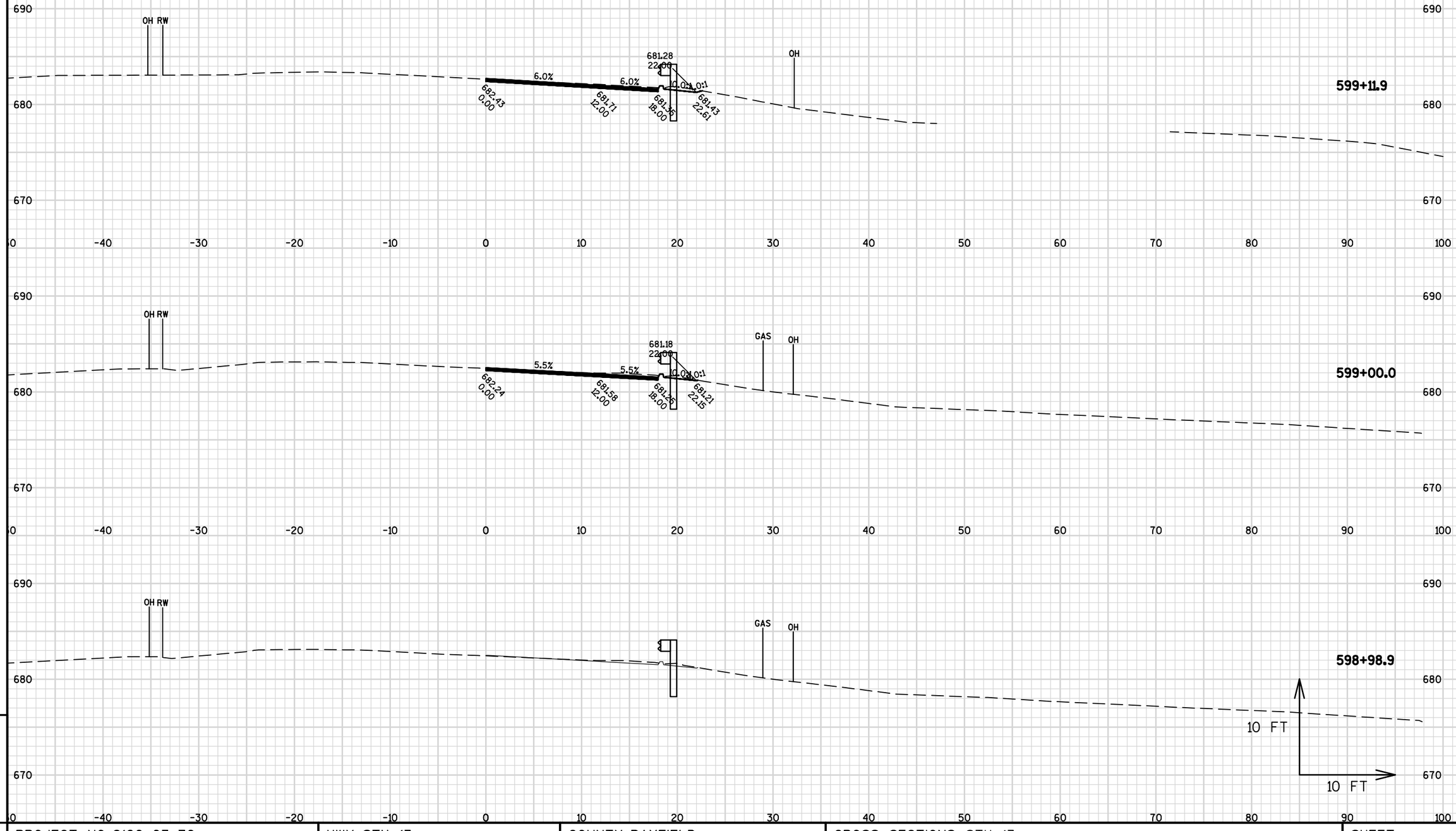
9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 256	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG PLOTTED : 1/17/2017 11:46 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090205 (4)

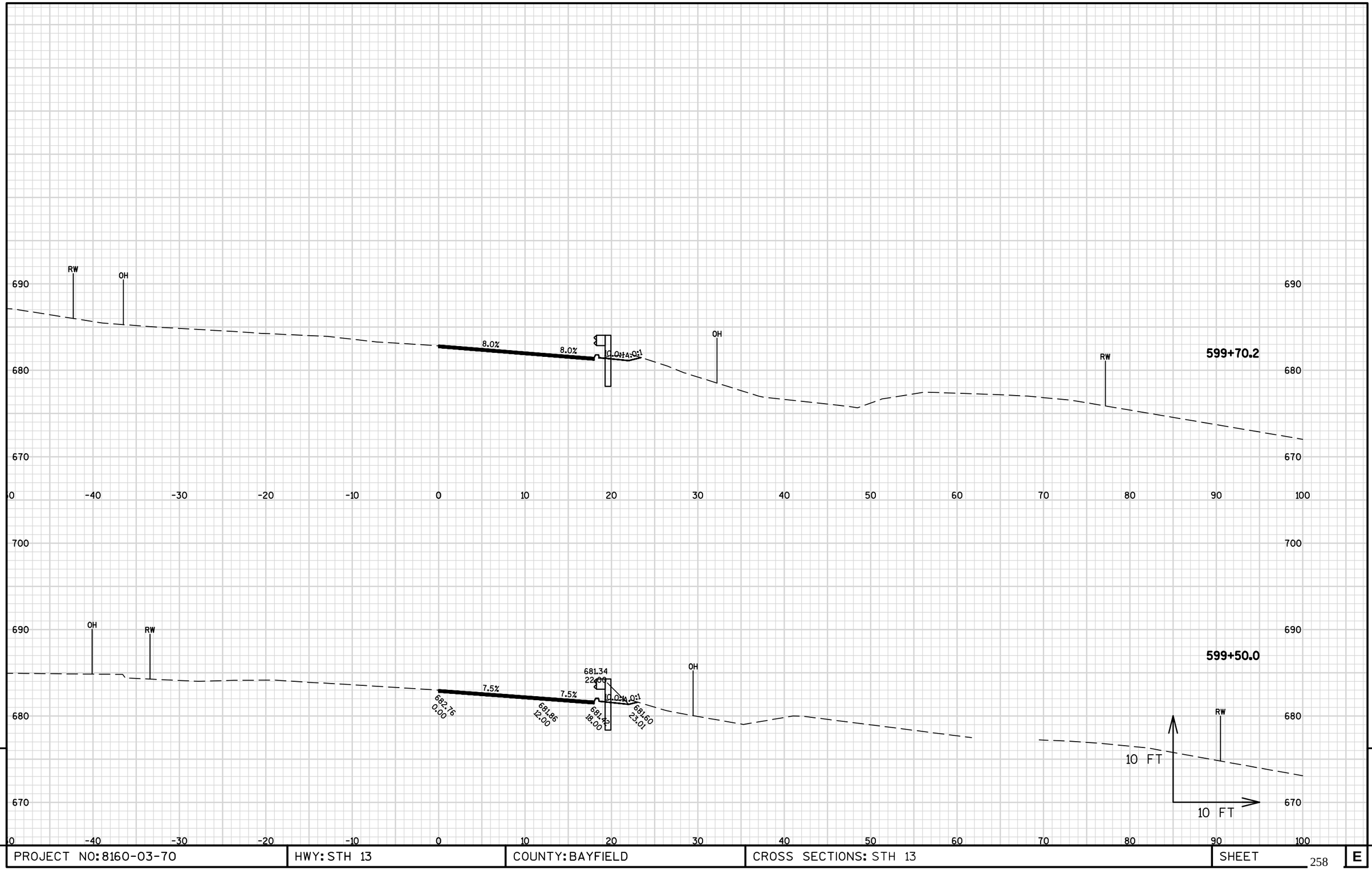
WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 257	E

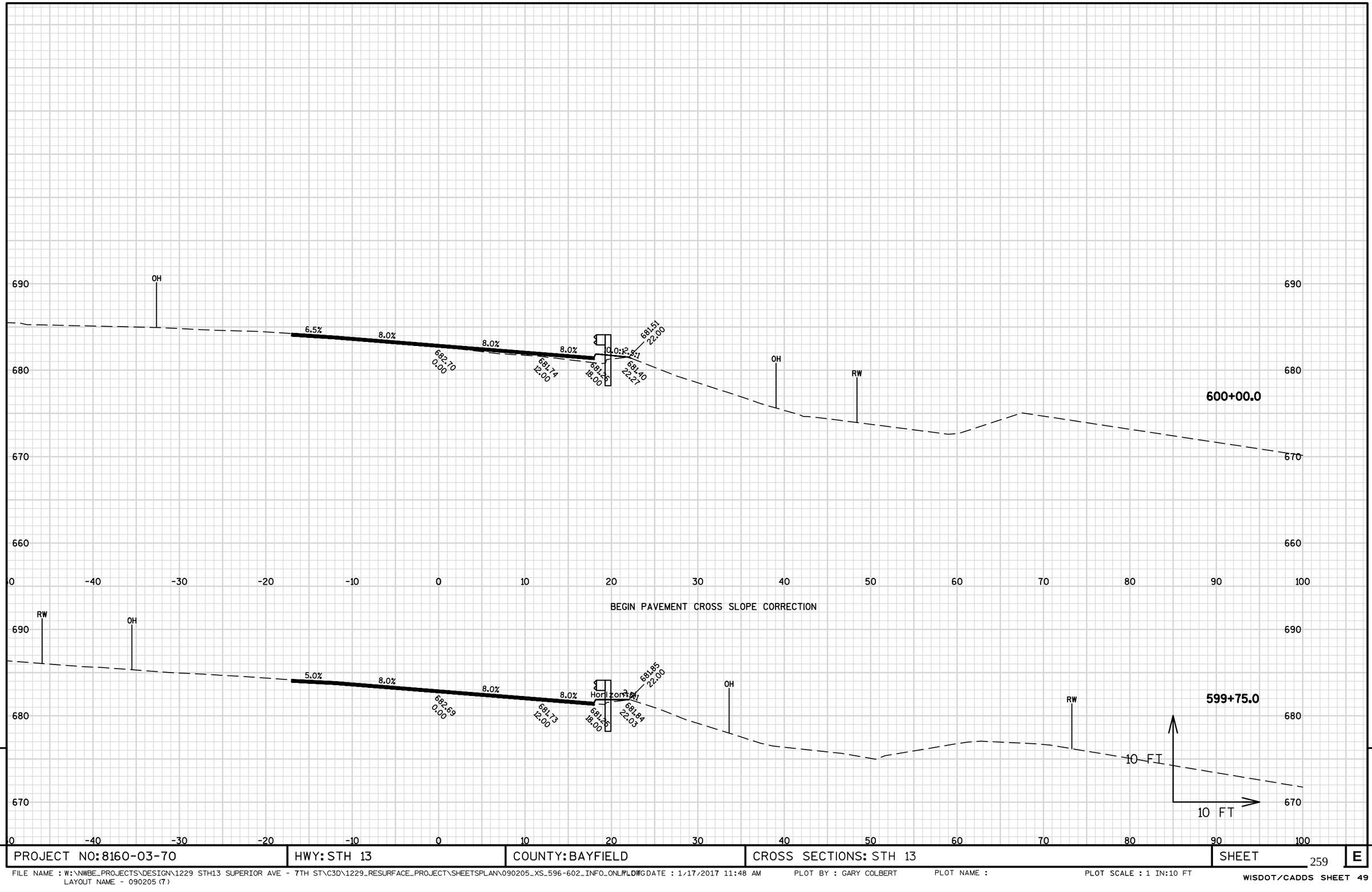


9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG
LAYOUT NAME - 090205 (6)
PLOTTED : 1/17/2017 11:47 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT



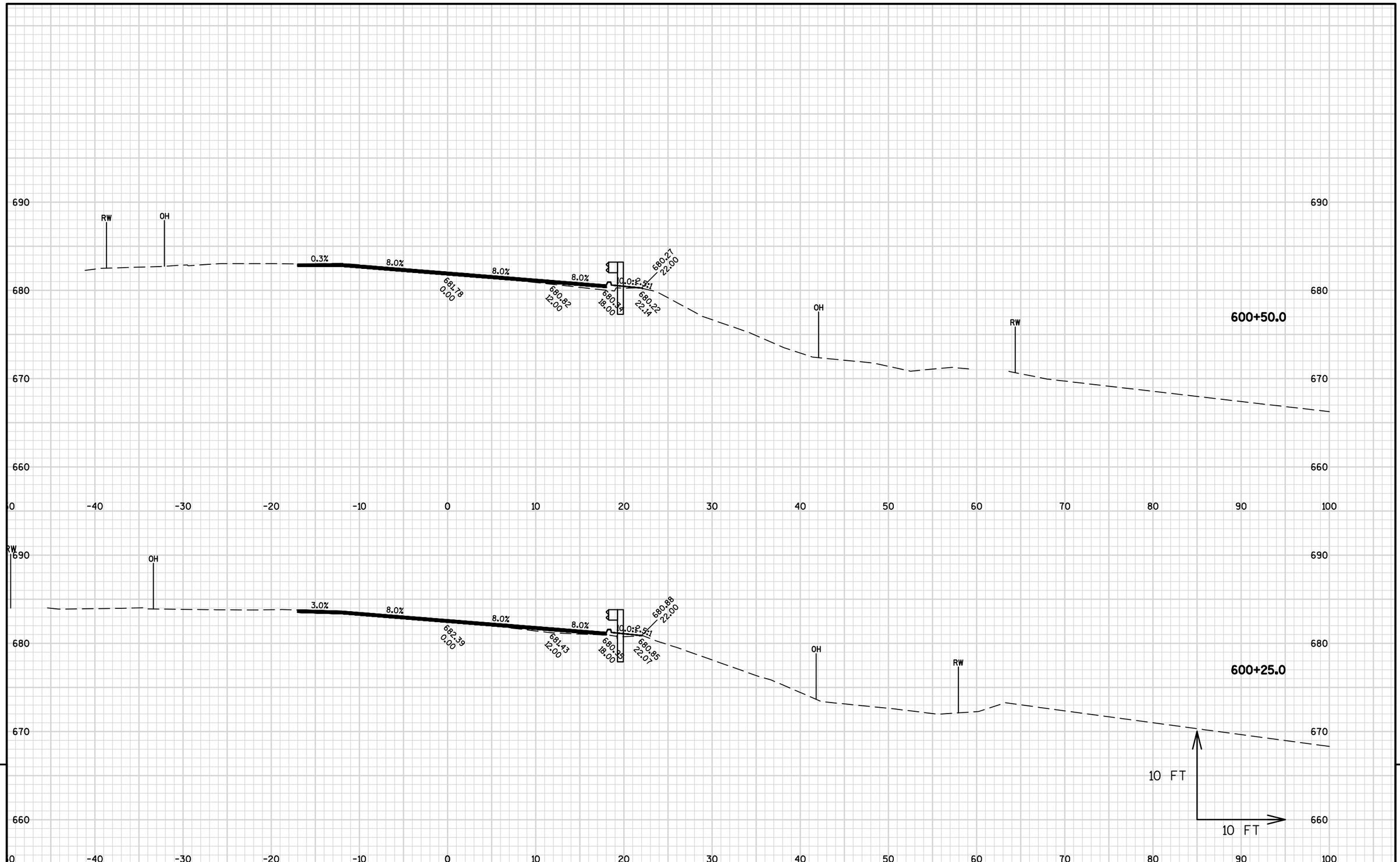
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 259	E

9

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG PLOTTED : 1/17/2017 11:48 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090205 (7)

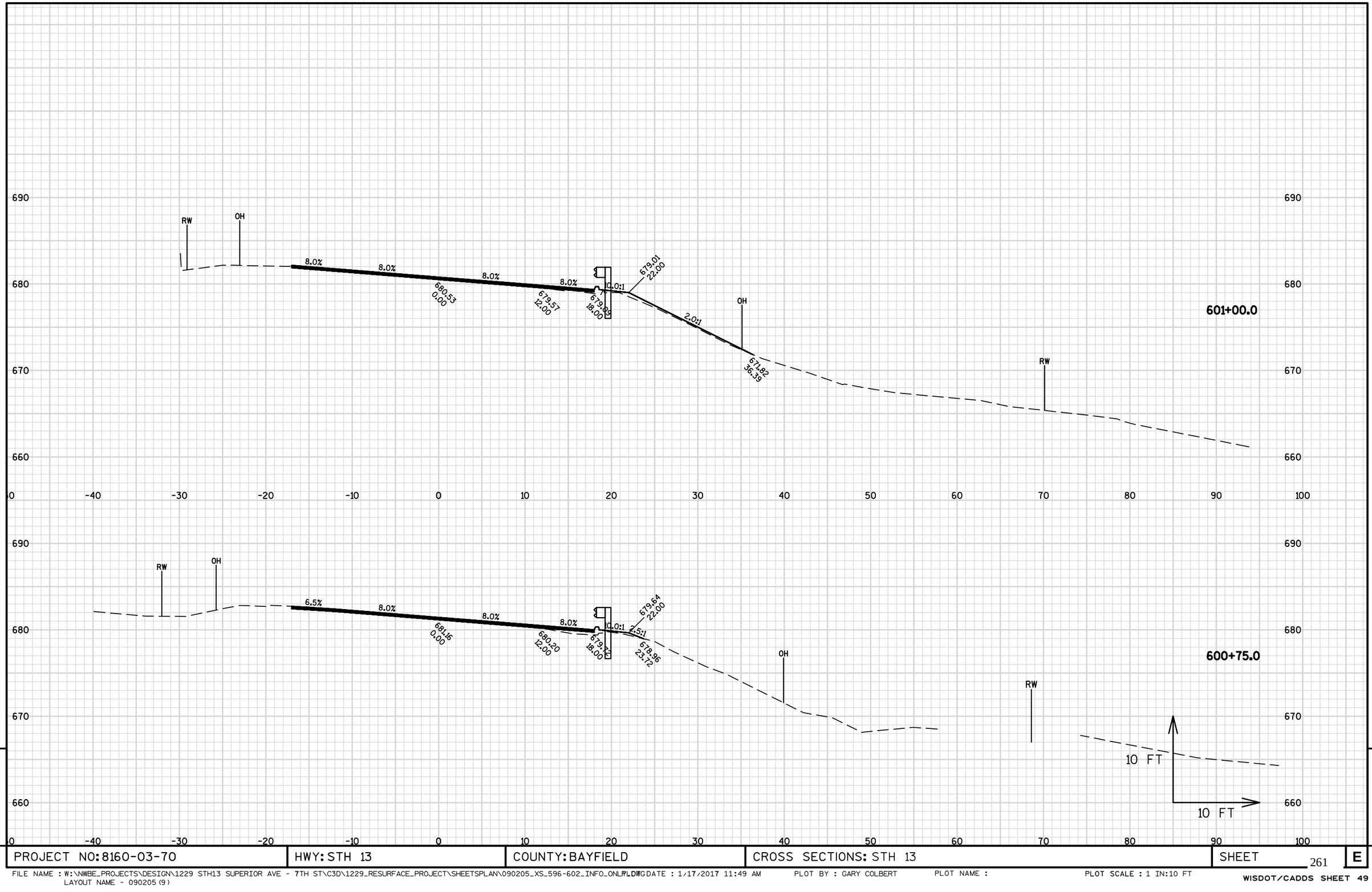
WISDOT/CADDS SHEET 49



PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 260 E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG DATE : 1/17/2017 11:48 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090205 (8)

WISDOT/CADDS SHEET 49



9

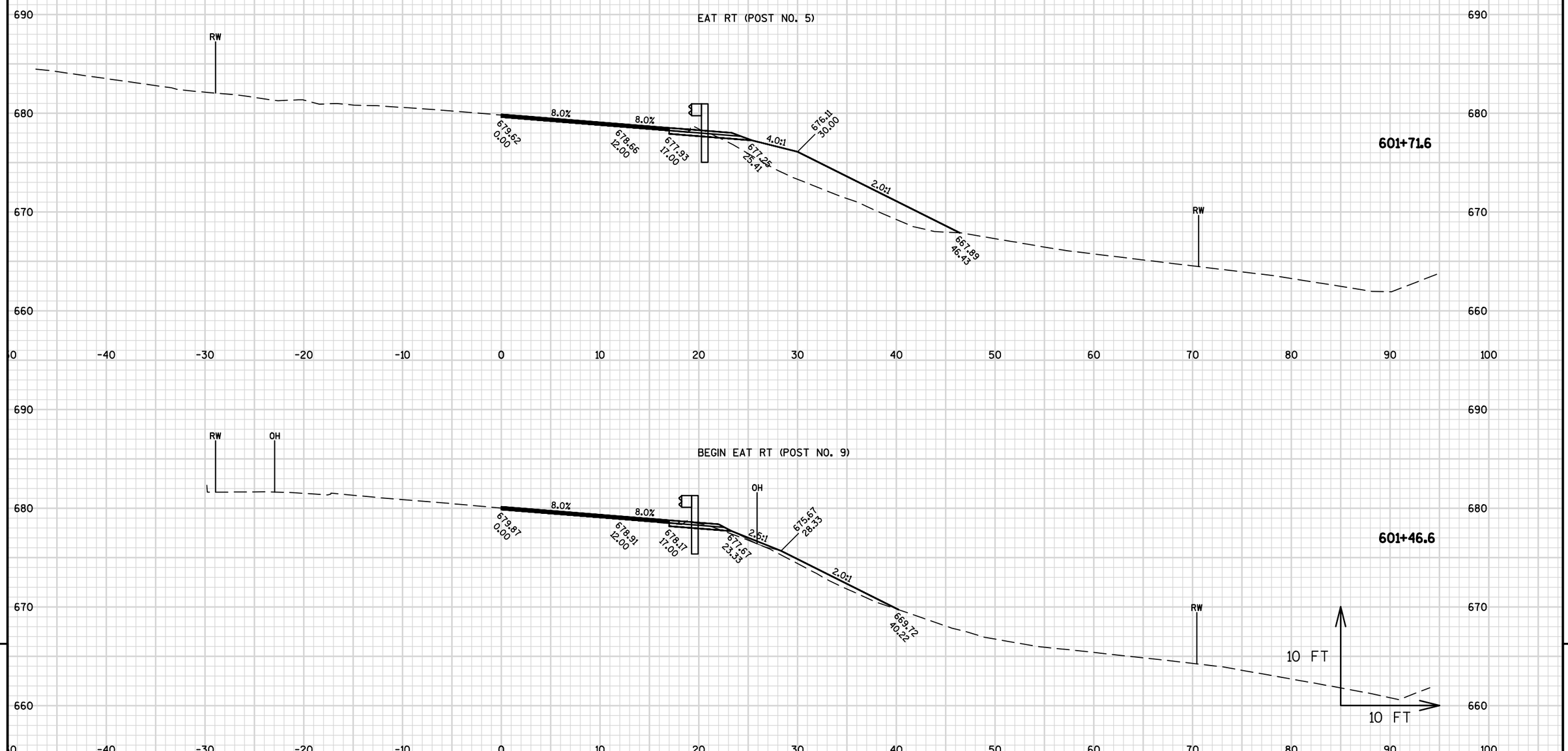
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG
LAYOUT NAME - 090205 (9)
PLOTTED : 1/17/2017 11:49 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 49

EAT RT (POST NO. 5)



BEGIN EAT RT (POST NO. 9)

9

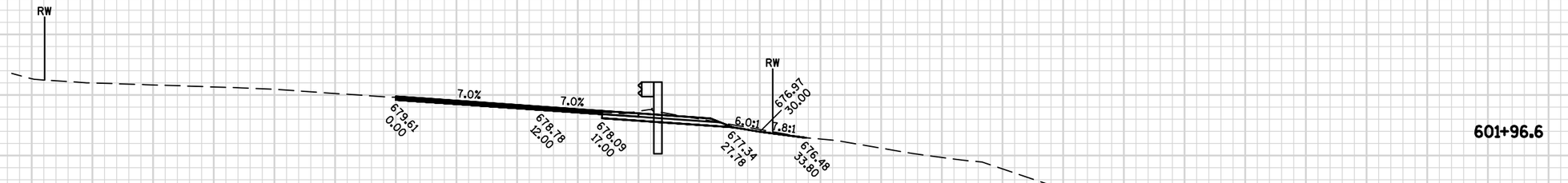
9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG
LAYOUT NAME - 090205 (10)
PLOTTED DATE : 1/17/2017 11:49 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT

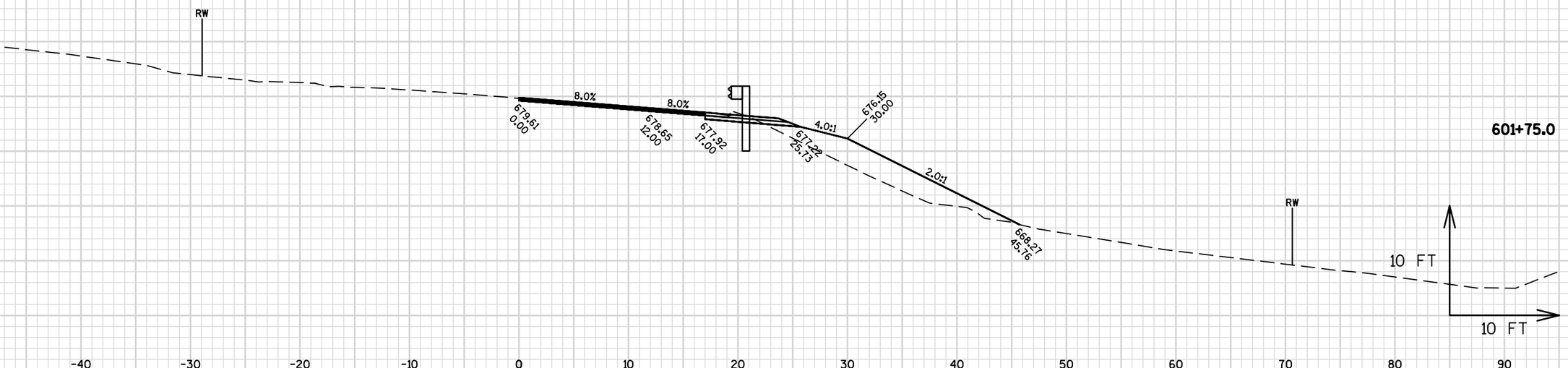
WISDOT/CADDS SHEET 49

END EAT RT (POST NO. 1)



601+96.6

END PAVEMENT CROSS SLOPE CORRECTION



601+75.0

9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 263	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG
LAYOUT NAME - 090205 (11)
PLOTTED : 1/17/2017 11:50 AM
PLOT BY : GARY COLBERT
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

690
680
670
RW
RW
602+50.0
-40 -30 -20 -10 0 10 20 30 40 50 60 70 80 90 100
690
680
670
RW RW
RW
602+25.0
-40 -30 -20 -10 0 10 20 30 40 50 60 70 80 90 100
690
680
670
STA. 602+10
END BARRIER SYSTEM GRADING SHAPING FINISHING RT
RW
RW
6.8%
6.8%
679.62
0.00
678.80
12.00
678.13
17.00
6.0:1
2.4:1
677.41
27.57
677.00
30.00
676.70
33.80
602+00.0
10 FT
10 FT
-40 -30 -20 -10 0 10 20 30 40 50 60 70 80 90 100
660

9

9

PROJECT NO: 8160-03-70	HWY: STH 13	COUNTY: BAYFIELD	CROSS SECTIONS: STH 13	SHEET 264	E

FILE NAME : W:\NWBE_PROJECTS\DESIGN\1229 STH13 SUPERIOR AVE - 7TH ST\C3D\1229_RESURFACE_PROJECT\SHEETSPLAN\090205_XS_596-602_INFO_ONLY.DWG PLOTTING DATE : 1/17/2017 11:50 AM PLOT BY : GARY COLBERT PLOT NAME : PLOT SCALE : 1 IN:10 FT
LAYOUT NAME - 090205 (12)

WISDOT/CADDS SHEET 49

Notes

Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

http://www.dot.wisconsin.gov