

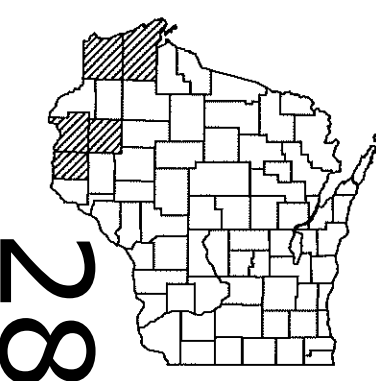
EAU
PROJECT ID: 1180-00-63, 1540-00-60, 1550-01-60, 8070-00-60,
WITH: N/A
8120-00-60, AND 8510-01-60
COUNTY: POLK, ST. CROIX, BAYFIELD,
DOUGLAS, AND BARRON

MAR 2017

ORDER OF SHEETS

- Section No. 1 Title
- Section No. 2 Typical Sections and Details
- Section No. 3 Estimate of Quantities
- Section No. 3 Miscellaneous Quantities
- Section No. 4 Right of Way Plat
- Section No. 5 Plan and Profile
- Section No. 6 Standard Detail Drawings
- Section No. 7 Sign Plates
- Section No. 8 Structure Plans
- Section No. 9 Computer Earthwork Data
- Section No. 9 Cross Sections

TOTAL SHEETS = 170



28

DESIGN DESIGNATION

- A.A.D.T. = N/A
- A.A.D.T. = N/A
- D.H.V. = N/A
- D.D. = N/A
- T. = N/A
- DESIGN SPEED = N/A
- ESALS = N/A



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STATE HIGHWAY REHABILITATION-MAINTENANCE PROJECT

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1180-00-63		
1540-00-60		
1550-01-60		
8070-00-60		
8120-00-60		
8510-01-60		

AMERY - MILLTON
(YORK PARK ENTRANCE TO USH 8)
STH 46
POLK COUNTY
STATE PROJECT NUMBER
8070-00-60

RIVER FALLS - NEW RICHMOND
(USH 12 TO CTH G)
STH 65
ST. CROIX COUNTY
STATE PROJECT NUMBER
1540-00-60

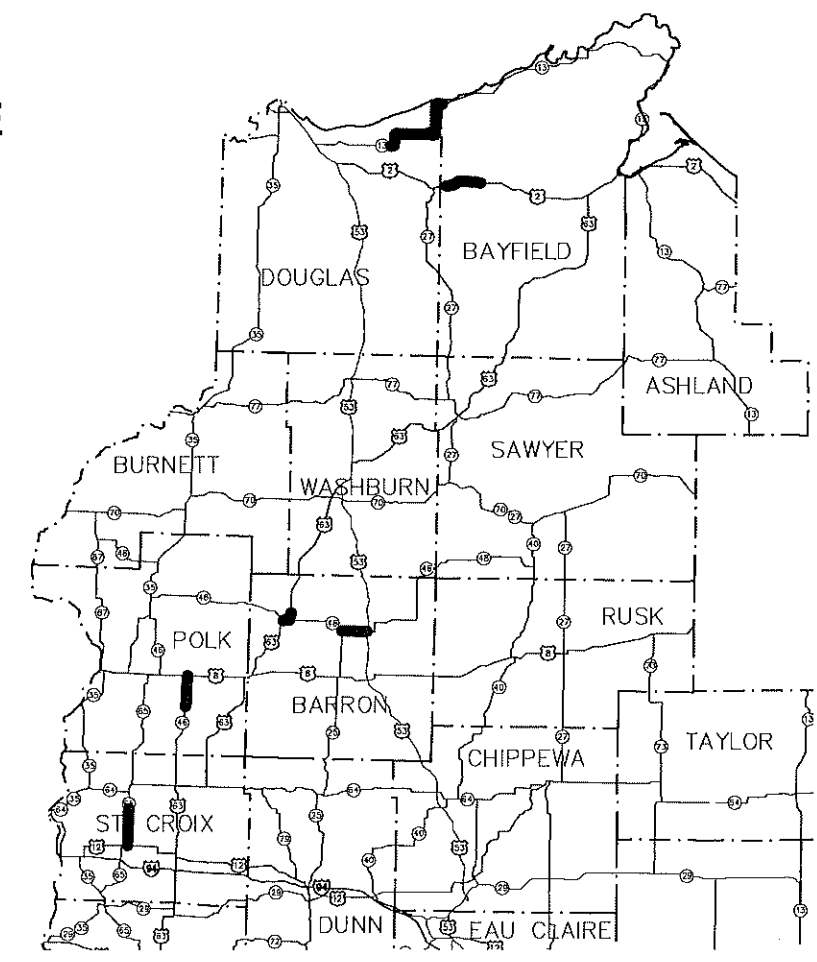
BRULE - INO
(SZNAIDER ROAD TO CTH A)
USH 2
BAYFIELD COUNTY
STATE PROJECT NUMBER
1180-00-63

PORT WING - SUPERIOR
(EAST COUNTY LINE TO CTH F)
STH 13
DOUGLAS COUNTY
STATE PROJECT NUMBER
8510-01-60

C CUMBERLAND, ELM ST AND 2ND AVE
(WEST JUNCTION STH 48 TO CHARRIE LN)
USH 63
BARRON COUNTY
STATE PROJECT NUMBER
1550-01-60

MCKINLEY - RICE LAKE
(STH 25 TO WEST AVENUE)
STH 48
BARRON COUNTY
STATE PROJECT NUMBER
8120-00-60

TOTAL NET LENGTH OF CENTERLINE = PROJECT 1180-00-63: 5.69 MI
PROJECT 1550-01-60: 2.37 MI
PROJECT 8510-01-60: 15.64 MI
PROJECT 8070-00-60: 4.95 MI
PROJECT 8120-00-60: 3.86 MI
PROJECT 1540-00-60: 5.47 MI
TOTAL = 37.98 MI



LAYOUT
SCALE 0 30 MI

ORIGINAL PLANS PREPARED BY
KL Engineering
[A] Better Experience

WISCONSIN
BRADLEY J. CUNNINGHAM
37935-006
MT. HOREB, WI
PROFESSIONAL ENGINEER

12-16-2016
(Date)
(Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor N/A
Designer KL ENGINEERING/WISDOT
Project Manager MOHAMAD HAYEK
Regional Examiner TOU YANG
Regional Supervisor MARK FLOEDERER

APPROVED FOR THE DEPARTMENT
DATE: 12/19/16
(Signature)

E

GENERAL NOTES

THE ENGINEER WILL DETERMINE ANY DETAILS OF CONSTRUCTION NOT SHOWN ON THE PLAN.

ALL WASTE MATERIAL RESULTING FROM THE VARIOUS OPERATIONS UNDER THIS CONTRACT SHALL BE COLLECTED TO BE PROPERLY DISPOSED OF PRIOR TO REOPENING LANES TO TRAFFIC.

THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

WHEN AN EXISTING STOP SIGN/SUPPORT IS TO BE REMOVED AND A NEW STOP SIGN/SUPPORT ERECTED, THE WORK SHALL BE DONE CONCURRENTLY. FOR ALL OTHER SIGNS THE REMOVAL OF THE EXISTING SIGN/SUPPORT AND ERECTION OF THE NEW SIGN/SUPPORT SHOULD BE DONE AS CONCURRENTLY AS POSSIBLE. IN NO CASE SHALL A NEW SIGN/SUPPORT BE DOWN FOR MORE THAN 24 HOURS AND THERE SHALL NOT BE MORE THAN ONE SIGN OF THE SAME LEGEND MISSING IN A ROW.

WOOD POST SIZES, FOR TYPE II SIGNING, ARE ESTIMATED LENGTHS AND THE ACTUAL LENGTH WILL BE DETERMINED IN THE FIELD.

THE NUMBER OF "NO PASSING ZONE" SIGNS (W-14-3) ARE ESTIMATED AND THE ACTUAL NUMBER WILL BE DETERMINED AFTER THE ROADWAYS HAVE BEEN SPOTTED.

NORTHWEST REGION TRAFFIC CONTACT

CHLOE ANDERSON
WISCONSIN DEPARTMENT OF TRANSPORTATION
5009 USH 53 SOUTH
EAU CLAIRE, WI 54701
PHONE: (715) 855-7672
CELL: (715) 577-4493

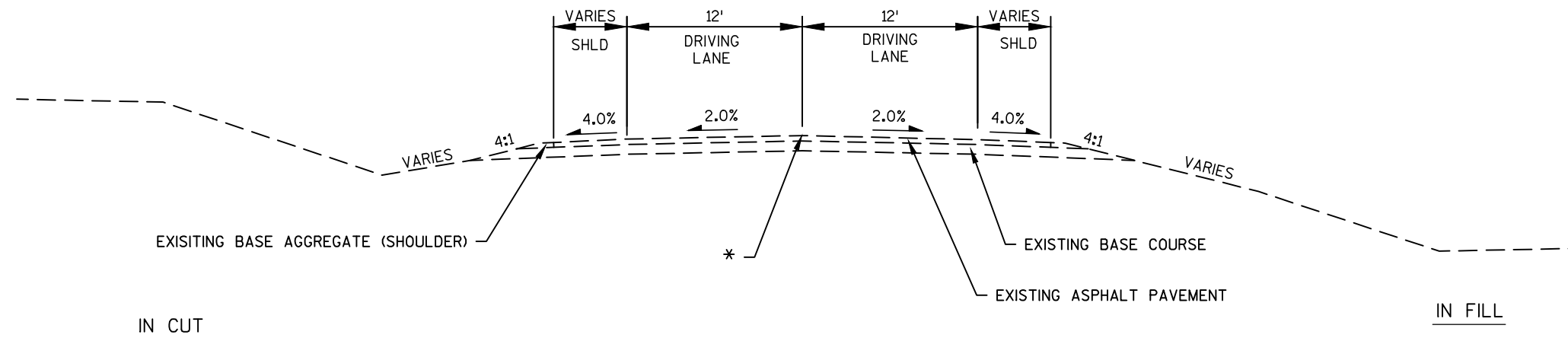
NORTHWEST REGION DNR CONTACTS

(ST. CROIX COUNTY)
CHRIS WILLGER
DNR WEST CENTRAL REGION HQ
1300 W. CLAIREMONT STREET
EAU CLAIRE, WI 54702
PHONE: (715) 839-1609

(BARRON, DOUGLAS, POLK COUNTY)
AMY CRONK
DNR NORTHERN REGION HQ
810 WEST MAPLE STREET
SPOONER, WI 54801
PHONE: (715) 635-4229

(BAYFIELD COUNTY)
SHAWN HASELEU
DNR NORTHERN REGION HQ
810 WEST MAPLE STREET
SPOONER, WI 54801
PHONE: (715) 635-4228

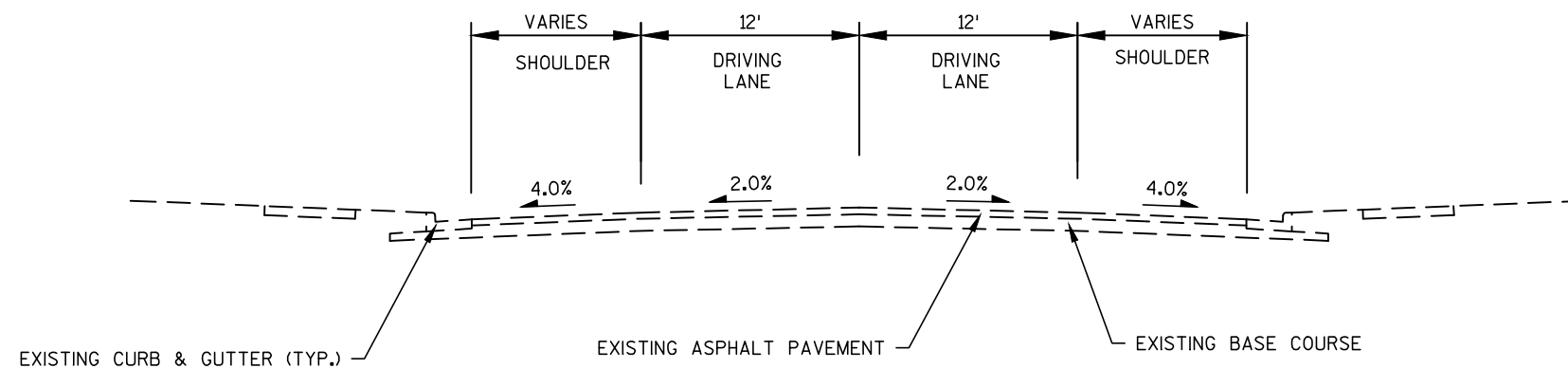




EXISTING TYPICAL SECTION - RURAL

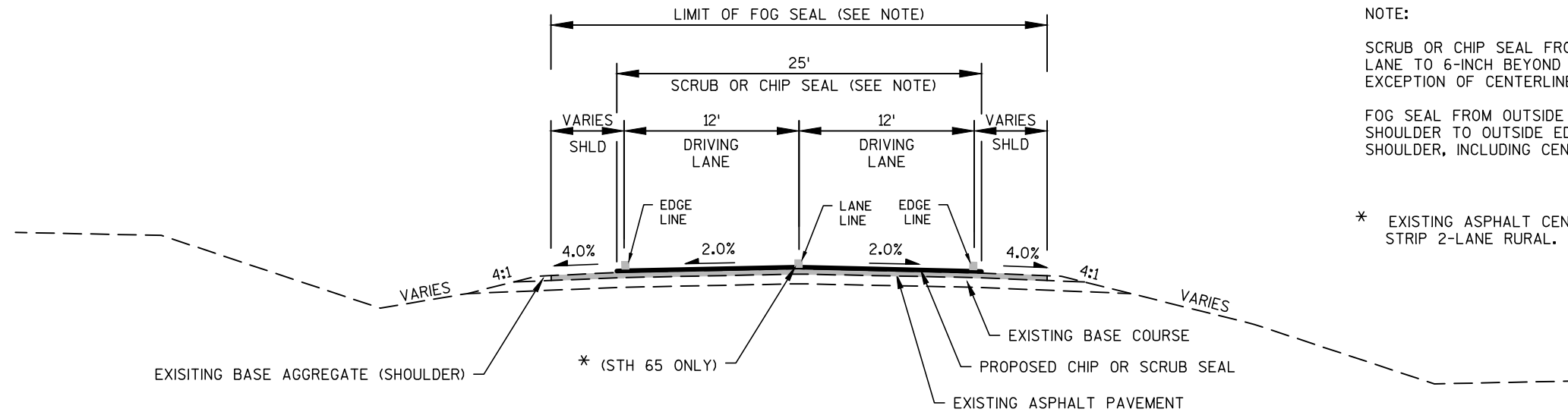
USH 63
STH 48
USH 2
STH 13
STH 46
STH 65

* EXISTING CENTER RUMBLE STRIPS - STH 65 ONLY



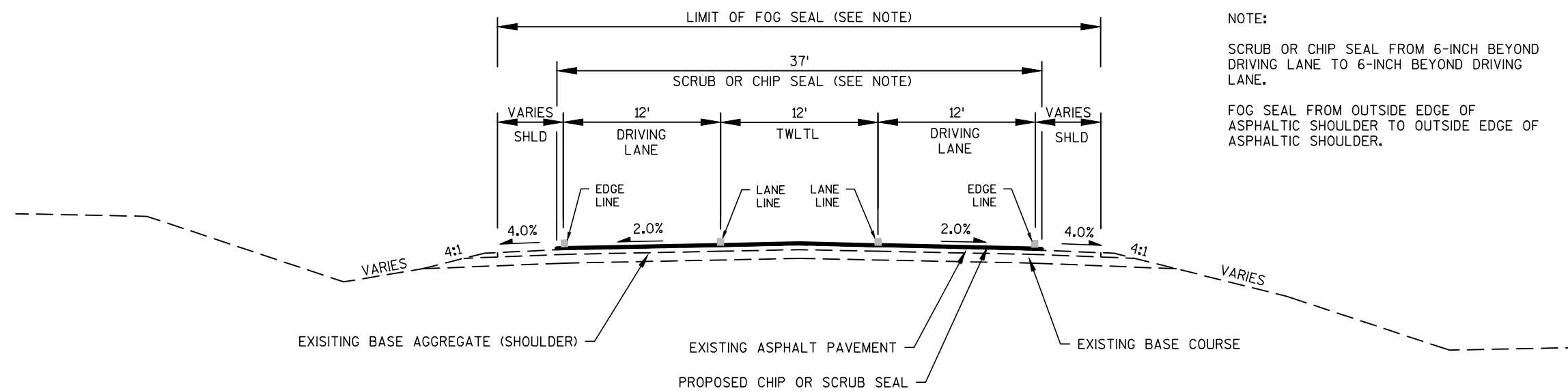
EXISTING TYPICAL SECTION - URBAN

USH 63



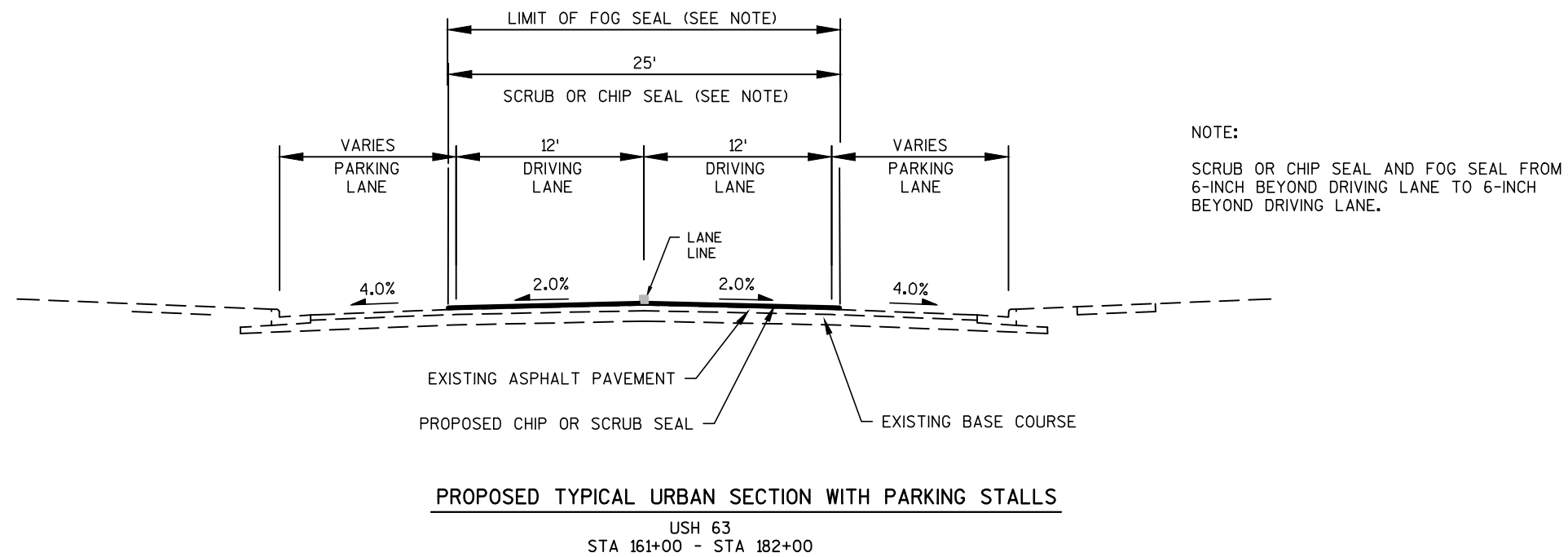
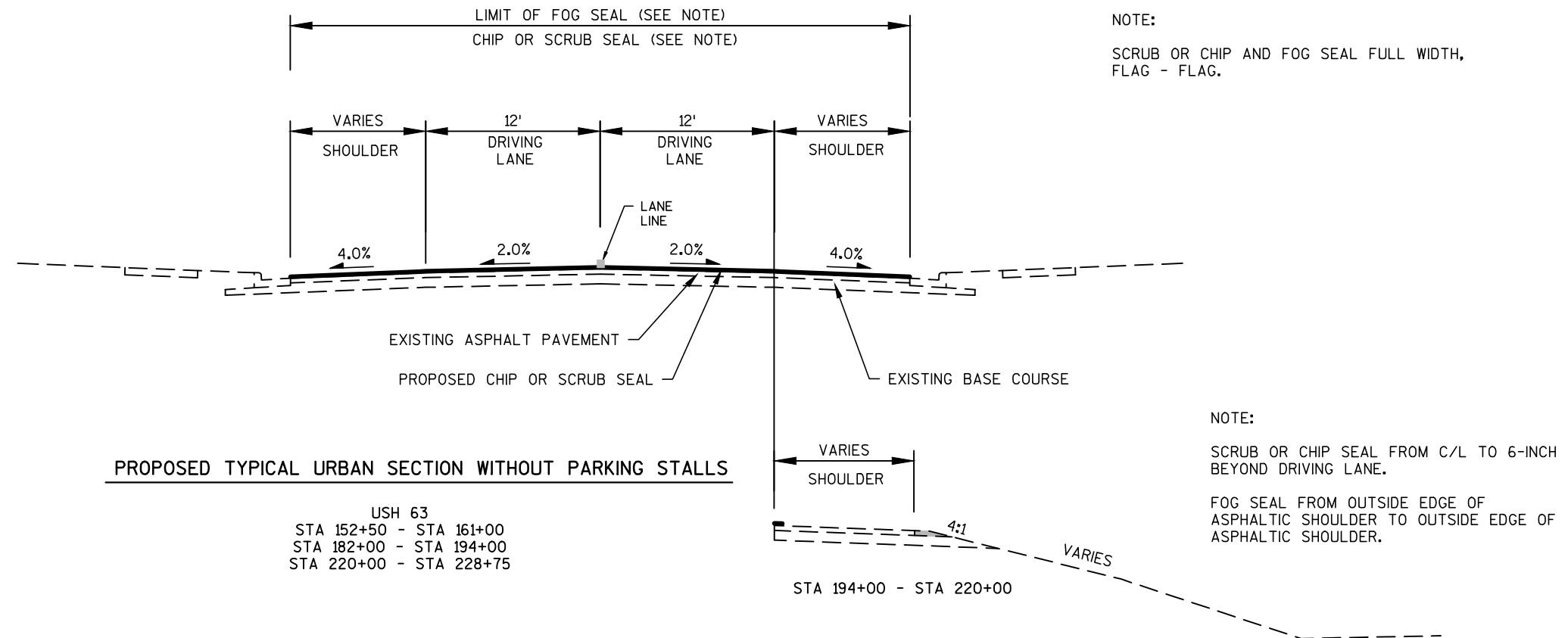
PROPOSED TYPICAL RURAL SECTION

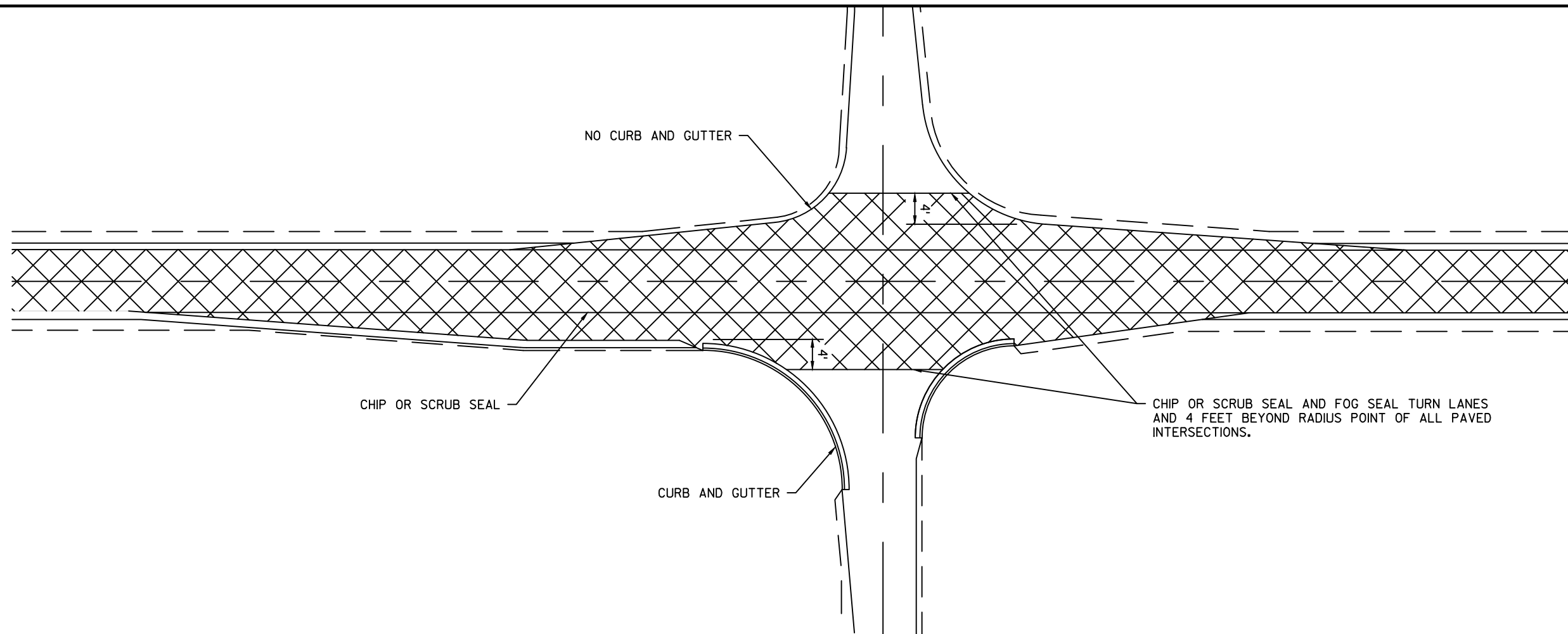
USH 63
STA 109+50 - STA 115+00
STA 228+75 - STA 232+00
STH 48
STA 122+00 - STA 325+75
USH 2
STA 132+88 - STA 433+30
STH 13
STA 126+00 - STA 952+00
STH 46
STA 112+25 - STA 373+50
STH 65
STA 112+25 - STA 401+25



PROPOSED TYPICAL RURAL SECTION

USH 63
STA 115+00 - STA 152+50





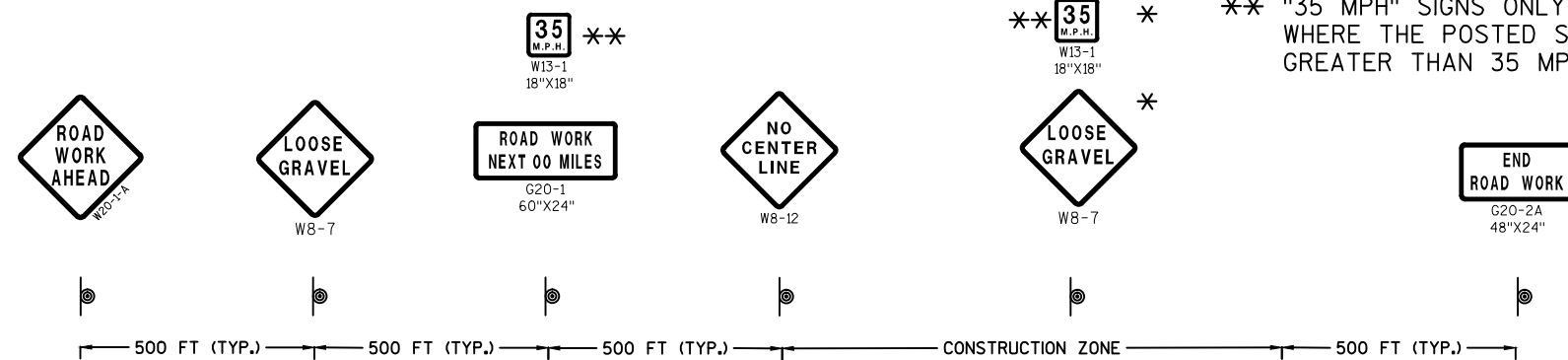
CHIP OR SCRUB SEAL LIMITS AT INTERSECTION DETAIL

NOTE: ALL SIGNS SHOWN ON THIS DETAIL WILL BE PAID FOR UNDER THE BID ITEM TRAFFIC CONTROL, PROJECT.

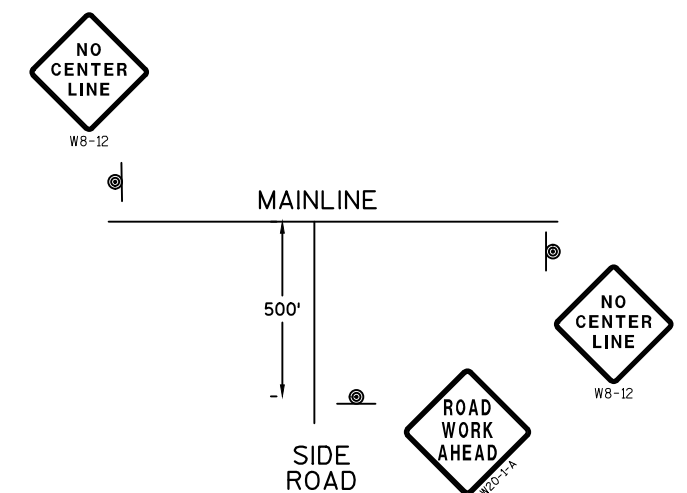
ALL SIGNS SHALL BE 48"X48" UNLESS OTHERWISE NOTED.

* "LOOSE GRAVEL" & "35 MPH" SIGNS LOCATED AT 1.0 MILE INTERVALS.

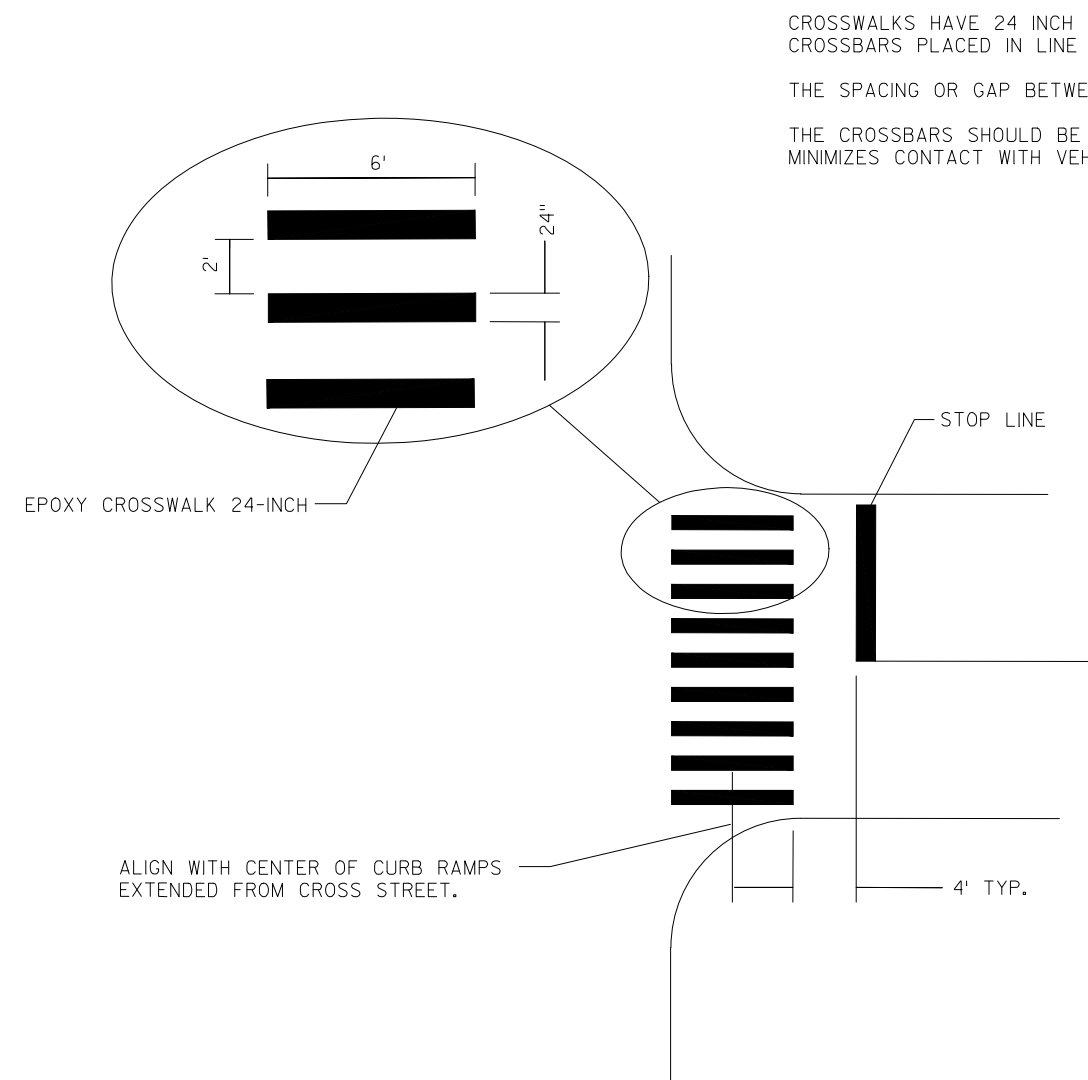
** "35 MPH" SIGNS ONLY REQUIRED WHERE THE POSTED SPEED IS GREATER THAN 35 MPH.



MAINLINE TRAFFIC CONTROL SIGN LAYOUT DETAIL



SIDE ROAD TRAFFIC CONTROL SIGN LAYOUT DETAIL



CROSS WALK & STOP LINES MARKING DETAIL
SEE PAVEMENT MARKING PLANS FOR LOCATIONS

Estimate Of Quantities

					1180-00-63	1540-00-60	1550-01-60	8070-00-60	8120-00-60	8510-01-60
Line	Item	Item Description	Unit	Total	Qty	Qty	Qty	Qty	Qty	Qty
0010	213.0100	Finishing Roadway (project) 01. 1180-00-63	EACH	1.000	1.000					
0020	213.0100	Finishing Roadway (project) 02. 1540-00-60	EACH	1.000		1.000				
0030	213.0100	Finishing Roadway (project) 03. 1550-01-60	EACH	1.000			1.000			
0040	213.0100	Finishing Roadway (project) 04. 8070-00-60	EACH	1.000				1.000		
0050	213.0100	Finishing Roadway (project) 05. 8120-00-60	EACH	1.000					1.000	
0060	213.0100	Finishing Roadway (project) 06. 8510-01-60	EACH	1.000						1.000
0070	618.0100	Maintenance And Repair of Haul Roads (project) 01. 1180-00-63	EACH	1.000	1.000					
0080	618.0100	Maintenance And Repair of Haul Roads (project) 02. 1540-00-60	EACH	1.000		1.000				
0090	618.0100	Maintenance And Repair of Haul Roads (project) 03. 1550-01-60	EACH	1.000			1.000			
0100	618.0100	Maintenance And Repair of Haul Roads (project) 04. 8070-00-60	EACH	1.000				1.000		
0110	618.0100	Maintenance And Repair of Haul Roads (project) 05. 8120-00-60	EACH	1.000					1.000	
0120	618.0100	Maintenance And Repair of Haul Roads (project) 06. 8510-01-60	EACH	1.000						1.000
0130	619.1000	Mobilization	EACH	1.000	0.190	0.110	0.070	0.130	0.110	0.390
0140	628.7015	Inlet Protection Type C	EACH	55.000	5.000	5.000	30.000	5.000	5.000	5.000
0150	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	147.000	32.000	10.000	48.000	30.000	27.000	
0160	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	165.000	46.000	21.000	14.000	40.000	44.000	
0170	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	17.000	10.000		4.000		3.000	
0180	637.2210	Signs Type II Reflective H	SF	1,676.700	414.800	159.220	536.700	331.120	234.860	
0190	637.2230	Signs Type II Reflective F	SF	646.000	153.750	42.000	171.000	95.500	183.750	
0200	638.2602	Removing Signs Type II	EACH	355.000	69.000	32.000	116.000	71.000	67.000	
0210	638.3000	Removing Small Sign Supports	EACH	301.000	71.000	31.000	66.000	65.000	68.000	
0220	643.0100	Traffic Control (project) 01. 1180-00-63	EACH	1.000	1.000					
0230	643.0100	Traffic Control (project) 02. 1540-00-60	EACH	1.000		1.000				
0240	643.0100	Traffic Control (project) 03. 1550-01-60	EACH	1.000			1.000			
0250	643.0100	Traffic Control (project) 04. 8070-00-60	EACH	1.000				1.000		
0260	643.0100	Traffic Control (project) 05. 8120-00-60	EACH	1.000					1.000	
0270	643.0100	Traffic Control (project) 06. 8510-01-60	EACH	1.000						1.000
0280	643.0410	Traffic Control Barricades Type II	DAY	20.000			20.000			
0290	643.0705	Traffic Control Warning Lights Type A	DAY	20.000			20.000			
0300	643.0900	Traffic Control Signs	DAY	20.000			20.000			
0310	646.0103	Pavement Marking Paint 4-Inch	LF	598,600.000	106,300.000	90,200.000	32,300.000	76,600.000	63,100.000	230,100.000
0320	646.0123	Pavement Marking Paint 8-Inch	LF	2,620.000	1,300.000	660.000	450.000	100.000		110.000
0330	646.0600	Removing Pavement Markings	LF	607,300.000	108,900.000	91,600.000	36,500.000	76,800.000	63,100.000	230,400.000
0340	647.0166	Pavement Marking Arrows Epoxy Type 2	EACH	25.000	2.000		22.000	1.000		
0350	647.0356	Pavement Marking Words Epoxy	EACH	3.000	2.000			1.000		

Estimate Of Quantities

					1180-00-63	1540-00-60	1550-01-60	8070-00-60	8120-00-60	8510-01-60
Line	Item	Item Description	Unit	Total	Qty	Qty	Qty	Qty	Qty	Qty
0360	647.0566	Pavement Marking Stop Line Epoxy 18-Inch	LF	58.000			46.000			12.000
0370	647.0606	Pavement Marking Island Nose Epoxy	EACH	1.000			1.000			
0380	647.0726	Pavement Marking Diagonal Epoxy 12-Inch	LF	86.000			86.000			
0390	647.0796	Pavement Marking Crosswalk Epoxy 24-Inch	LF	546.000			546.000			
0400	647.0955	Removing Pavement Markings Arrows	EACH	25.000	2.000		22.000	1.000		
0410	647.0965	Removing Pavement Markings Words	EACH	3.000	2.000			1.000		
0420	648.0100	Locating No-Passing Zones	MI	35.110	5.680	5.470	0.230	4.910	3.860	14.960
0430	649.0402	Temporary Pavement Marking Paint 4-Inch	LF	154,900.000	47,600.000	33,700.000	24,600.000	25,900.000	23,100.000	
0440	649.2102	Temporary Raised Pavement Markers Type II	EACH	55,800.000	11,500.000	9,200.000	2,300.000	7,700.000	6,500.000	18,600.000
0450	SPV.0180	Special 01. Chip Seal	SY	136,500.000		83,200.000	53,300.000			
0460	SPV.0180	Special 02. Scrub Seal	SY	458,000.000	104,800.000			73,500.000	57,800.000	221,900.000
0470	SPV.0180	Special 03. Fog Seal	SY	707,500.000	120,800.000	99,100.000	56,400.000	87,800.000	69,200.000	274,200.000

3

FINISHING ROADWAY (1180-00-63)

CAT	STATION	TO	STATION	LOCATION	213.0100 EACH	REMARKS
0010	133+00	-	433+00	USH 2	1	
TOTAL PROJECT ID 1180-00-63					1	

FINISHING ROADWAY (1540-00-60)

CAT	STATION	TO	STATION	LOCATION	213.0100 EACH	REMARKS
0010	112+25	-	401+25	STH 65	1	
TOTAL PROJECT ID 1540-00-60					1	

FINISHING ROADWAY (1550-01-60)

CAT	STATION	TO	STATION	LOCATION	213.0100 EACH	REMARKS
0010	109+50	-	232+00	USH 63	1	
TOTAL PROJECT ID 1550-01-60					1	

FINISHING ROADWAY (8070-00-60)

CAT	STATION	TO	STATION	LOCATION	213.0100 EACH	REMARKS
0010	113+00	-	372+40	STH 46	1	
TOTAL PROJECT ID 8070-00-60					1	

FINISHING ROADWAY (8120-00-60)

CAT	STATION	TO	STATION	LOCATION	213.0100 EACH	REMARKS
0010	122+00	-	325+75	STH 48	1	
TOTAL PROJECT ID 8120-00-60					1	

FINISHING ROADWAY (8510-01-60)

CAT	STATION	TO	STATION	LOCATION	213.0100 EACH	REMARKS
0010	126+00	-	915+75	STH 13	1	
TOTAL PROJECT ID 8510-01-60					1	

MAINTENANCE AND REPAIR OF HAUL ROADS (1180-00-63)

CAT	STATION	TO	STATION	LOCATION	618.0100 EACH	REMARKS
0010	133+00	-	433+00	USH 2	1	
TOTAL PROJECT ID 1180-00-63					1	

MAINTENANCE AND REPAIR OF HAUL ROADS (1540-00-60)

CAT	STATION	TO	STATION	LOCATION	618.0100 EACH	REMARKS
0010	112+25	-	401+25	STH 65	1	
TOTAL PROJECT ID 1540-00-60					1	

MAINTENANCE AND REPAIR OF HAUL ROADS (1550-01-60)

CAT	STATION	TO	STATION	LOCATION	618.0100 EACH	REMARKS
0010	109+50	-	232+00	USH 63	1	
TOTAL PROJECT ID 1550-01-60					1	

MAINTENANCE AND REPAIR OF HAUL ROADS (8070-00-60)

CAT	STATION	TO	STATION	LOCATION	618.0100 EACH	REMARKS
0010	113+00	-	372+40	STH 46	1	
TOTAL PROJECT ID 8070-00-60					1	

MAINTENANCE AND REPAIR OF HAUL ROADS (8120-00-60)

CAT	STATION	TO	STATION	LOCATION	618.0100 EACH	REMARKS
0010	122+00	-	325+75	STH 48	1	
TOTAL PROJECT ID 8120-00-60					1	

MAINTENANCE AND REPAIR OF HAUL ROADS (8510-01-60)

CAT	STATION	TO	STATION	LOCATION	618.0100 EACH	REMARKS
0010	126+00	-	915+75	STH 13	1	
TOTAL PROJECT ID 8510-01-60					1	

TRAFFIC CONTROL (1180-00-63)

CAT	STATION	TO	STATION	LOCATION	643.0100 EACH	REMARKS
0010	133+00	-	433+00	USH 2	1	
TOTAL PROJECT ID 1180-00-63					1	

TRAFFIC CONTROL (1540-00-60)

CAT	STATION	TO	STATION	LOCATION	643.0100 EACH	REMARKS
0010	112+25	-	401+25	STH 65	1	
TOTAL PROJECT ID 1540-00-60					1	

TRAFFIC CONTROL (1550-01-60)

CAT	STATION	TO	STATION	LOCATION	643.0100 EACH	REMARKS
0010	109+50	-	232+00	USH 63	1	
TOTAL PROJECT ID 1550-01-60					1	

TRAFFIC CONTROL (8070-00-60)

CAT	STATION	TO	STATION	LOCATION	643.0100 EACH	REMARKS
0010	113+00	-	372+40	STH 46	1	
TOTAL PROJECT ID 8070-00-60					1	

TRAFFIC CONTROL (8120-00-60)

CAT	STATION	TO	STATION	LOCATION	643.0100 EACH	REMARKS
0010	122+00	-	325+75	STH 48	1	
TOTAL PROJECT ID 8120-00-60					1	

TRAFFIC CONTROL (8510-01-60)

CAT	STATION	TO	STATION	LOCATION	643.0100 EACH	REMARKS
0010	126+00	-	915+75	STH 13	1	
TOTAL PROJECT ID 8510-01-60					1	

3

3

PAVEMENT MARKING PAINT								
					646.0103	646.0103	646.0123	
					PAVEMENT	PAVEMENT	PAVEMENT	
					MARKING	MARKING	MARKING	
					PAINT	PAINT	PAINT	
					4-INCH	4-INCH	8-INCH	
					(YELLOW)	(WHITE)	(WHITE)	
CAT	STATION	TO	STATION	LOCATION	LF	LF	LF	REMARKS
0010	133+00	-	433+00	USH 2	40,200	66,100	1,300	
TOTAL PROJECT ID 1180-00-63					106,300		1,300	
0010	112+25	-	401+25	STH 65	33,700	56,500	660	
TOTAL PROJECT ID 1540-00-60					90,200		660	
0010	109+50	-	232+00	USH 63	24,600	7,700	450	
TOTAL PROJECT ID 1550-01-60					32,300		450	
0010	113+00	-	372+40	STH 46	25,800	50,800	100	
TOTAL PROJECT ID 8070-00-60					76,600		100	
0010	122+00	-	325+75	STH 48	23100	40000		
TOTAL PROJECT ID 8120-00-60					63100		0	
0010	126+00	-	915+75	STH 13	75,500	154,600	110	
TOTAL PROJECT ID 8510-01-60					230,100		110	
GRAND TOTAL					598,600		2,620	

3

PAVEMENT MARKING (SPECIAL MARKINGS)						
COUNTY	BARRON					
PROJECT ID	1550-01-60					
MAP REFERENCE HWY	USH 63					
MAP REFERENCE SITE ID	700 FT NORTH OF STH 48 WEST - CUMBERLAND					
BID ITEM #	DESCRIPTION	MARKING LOCATION	UNIT	USH 63 NB	USH 63 SB	TOTAL QUANTITY
647.0726	PAVEMENT MARKING, DIAGONAL, EPOXY, 12-INCH	MEDIAN ISLAND	L.F.	50	36	86
TOTAL PROJECT ID 1550-01-60						86
PROJECT ID	1550-01-60					
MAP REFERENCE HWY	USH 63					
MAP REFERENCE SITE ID	900 FT NORTH OF STH 48 WEST - CUMBERLAND					
BID ITEM #	DESCRIPTION	MARKING LOCATION	UNIT	USH 63 NB	USH 63 SB	TOTAL QUANTITY
647.0166	PAVEMENT MARKING, ARROWS, EPOXY, TYPE 2	TWLTL - TWO WAY LEFT TURN LANE	EACH	11	11	22
TOTAL PROJECT ID 1550-01-60						22
PROJECT ID	1550-01-60					
MAP REFERENCE HWY	USH 63					
MAP REFERENCE SITE ID	5TH AVE - CUMBERLAND					
BID ITEM #	DESCRIPTION	MARKING LOCATION	UNIT	USH 63 NB	USH 63 SB	TOTAL QUANTITY
647.0606	PAVEMENT MARKING, ISLAND NOSE, EPOXY	MEDIAN ISLAND	EACH	1		1
TOTAL PROJECT ID 1550-01-60						1
PROJECT ID	1550-01-60					
MAP REFERENCE HWY	USH 63					
MAP REFERENCE SITE ID	STH 48 EAST - CUMBERLAND					
BID ITEM #	DESCRIPTION	MARKING LOCATION	UNIT	USH 63 NB	USH 63 SB	TOTAL QUANTITY
647.0566	PAVEMENT MARKING, STOP LINE, EPOXY, 18-INCH	MAINLINE	L.F.	21	25	46
TOTAL PROJECT ID 1550-01-60						46
PROJECT ID	1550-01-60					
MAP REFERENCE HWY	USH 63					
BID ITEM #	DESCRIPTION	LOCATION	UNIT	USH 63 NB	USH 63 SB	TOTAL QUANTITY
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 / EIGHTH ST - CUMBERLAND	L.F.	78		78
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 / ELM ST - CUMBERLAND	L.F.	48	42	90
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 / SORENSEN ST - CUMBERLAND	L.F.	42	42	84
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 / GROVE ST - CUMBERLAND	L.F.	42	42	84
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 / FOSTER ST - CUMBERLAND	L.F.		42	42
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 / WATER ST - CUMBERLAND	L.F.	42	42	84
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 / MIDBLOCK - CUMBERLAND	L.F.	42		42
647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH	USH 63 AND VETERANS ST CUMBERLAND	L.F.	42		42
TOTAL PROJECT ID 1550-01-60						546
COUNTY	POLK					
PROJECT ID	8070-00-60					
MAP REFERENCE HWY	STH 46					
MAP REFERENCE SITE ID						
BID ITEM #	DESCRIPTION	MARKING LOCATION	UNIT	STH 46 WB	STH 46 EB	TOTAL QUANTITY
647.0166	PAVEMENT MARKING, ARROWS, EPOXY, TYPE 2	RIGHT TURN LANE	E.A.	1		1
647.0356	PAVEMENT MARKING, WORDS, EPOXY "ONLY"	RIGHT TURN LANE	E.A.	1		1
TOTAL PROJECT ID 8070-00-60						
COUNTY	BAYFIELD					
PROJECT ID	1180-00-63					
MAP REFERENCE HWY	USH 2					
MAP REFERENCE SITE ID	HUGHES TOWN HALL ROAD & RANGE LINE ROAD					
BID ITEM #	DESCRIPTION	MARKING LOCATION	UNIT	USH 2 WB	USH 2 EB	TOTAL QUANTITY
647.0166	PAVEMENT MARKING, ARROWS, EPOXY, TYPE 2	RIGHT TURN LANES	E.A.	1	1	2
647.0356	PAVEMENT MARKING, WORDS, EPOXY "ONLY"	RIGHT TURN LANES	E.A.	1	1	2
TOTAL PROJECT ID 1180-00-63						
COUNTY	DOUGLAS					
PROJECT ID	8510-01-60					
MAP REFERENCE HWY	STH 13					
MAP REFERENCE SITE ID	STH 13 & CTH H INTERSECTION					
BID ITEM #	DESCRIPTION	MARKING LOCATION	UNIT	STH 13 NB	STH 13 SB	TOTAL QUANTITY
647.0566	PAVEMENT MARKING, STOP LINE, EPOXY, 18-INCH	MAINLINE	L.F.		12	12
TOTAL PROJECT ID 8510-01-60						12

3

LOCATING NO-PASSING ZONES						
					648.0100	
CAT	STATION	TO	STATION	LOCATION	MI	REMARKS
0010	133+00	-	433+00	USH 2	5.68	
TOTAL PROJECT ID 1180-00-63					5.68	
0010	112+25	-	401+25	STH 65	5.47	
TOTAL PROJECT ID 1540-00-60					5.47	
0010	109+50	-	220+00	USH 63	---	
0010	220+00	-	232+00	USH 63	0.23	
TOTAL PROJECT ID 1550-01-60					0.23	
0010	113+00	-	372+40	STH 46	4.91	
TOTAL PROJECT ID 8070-00-60					4.91	
0010	122+00	-	325+75	STH 48	3.86	
TOTAL PROJECT ID 8120-00-60					3.86	
0010	126+00	-	915+75	STH 13	14.96	
TOTAL PROJECT ID 8510-01-60					14.96	
GRAND TOTAL					35.11	
PAVEMENT MARKING SUMMARY (SPECIAL MARKINGS)						
CAT	ITEM NUMBER	BID ITEM DESCRIPTION			UNIT	TOTALS
0010	647.0166	PAVEMENT MARKING, ARROW, EPOXY, TYPE 2			EACH	25
0010	647.0356	PAVEMENT MARKING, EPOXY, WORDS "ONLY"			EACH	3
0010	647.0566	PAVEMENT MARKING, STOP LINE, EPOXY, 18-INCH			L.F.	58
0010	647.0606	PAVEMENT MARKING, ISLAND NOSE, EPOXY			EACH	1
0010	647.0726	PAVEMENT MARKING, DIAGONAL, EPOXY, 12-INCH			L.F.	86
0010	647.0796	PAVEMENT MARKING, CROSSWALK, EPOXY, 24-INCH			L.F.	546

TEMPORARY PAVEMENT MARKING 4-INCH

CAT	STATION	TO	STATION	LOCATION		649.0402	649.0402	REMARKS
						(YELLOW)	(WHITE)	
0010	133+00	-	433+00	USH 2	*	LF	LF	
						40,200	--	CENTERLINE
						--	7,400	CLIMBING LANE SKIPS
TOTAL PROJECT ID 1180-00-63						47,600		
0010	112+25	-	401+25	STH 65	*	33,700	--	CENTERLINE
						33,700		
						TOTAL PROJECT ID 1540-00-60		
0010	109+50	-	232+00	USH 63	*	24,600	--	CENTERLINE
						24,600		
						TOTAL PROJECT ID 1550-01-60		
0010	113+00	-	372+40	STH 46	*	25,800	--	CENTERLINE
						--	100	BYPASS LANE SKIPS
						25,900		
TOTAL PROJECT ID 8070-00-60								
0010	122+00	-	325+75	STH 48	*	23,100	--	CENTERLINE
						23,100		
						TOTAL PROJECT ID 8120-00-60		
0010	126+00	-	915+75	STH 13	**	--	--	
						0		
						TOTAL PROJECT ID 8510-01-60		
GRAND TOTAL						154,900		

* TEMPORARY RAISED PAVEMENT MARKERS ARE ACCEPTABLE FOR A MAXIMUM OF 3 DAYS AFTER THE SEAL COAT IS PLACED. UNTIL THE PERMANENT MARKINGS ARE PLACED, TEMPORARY PAVEMENT MARKINGS ARE REQUIRED FOR CENTER LINES, LANE LINES AND EDGE LINES. (DUE TO AADT LESS THAN 3,500)

** TEMPORARY RAISED PAVEMENT MARKINGS NOT REQUIRED UNTIL PERMANENT PAVEMENT MARKINGS ARE PLACED. (DUE TO AADT LESS THAN 3,500)

TEMPORARY RAISED PAVEMENT MARKERS TYPE II

CAT	STATION	TO	STATION	LOCATION	649.2102	REMARKS
					EACH	
0010	133+00	-	433+00	USH 2	7,500	YELLOW
0010	133+00	-	433+00	USH 2	4,000	WHITE
TOTAL PROJECT ID 1180-00-63					11,500	
0010	112+25	-	401+25	STH 65	6,900	YELLOW
0010	112+25	-	401+25	STH 65	2,300	WHITE
TOTAL PROJECT ID 1540-00-60					9,200	
0010	109+50	-	232+00	USH 63	2,000	YELLOW
0010	109+50	-	232+00	USH 63	300	WHITE
TOTAL PROJECT ID 1550-01-60					2,300	
0010	113+00	-	372+40	STH 46	5,600	YELLOW
0010	113+00	-	372+40	STH 46	2,100	WHITE
TOTAL PROJECT ID 8070-00-60					7,700	
0010	122+00	-	325+75	STH 48	4,900	YELLOW
0010	122+00	-	325+75	STH 48	1,600	WHITE
TOTAL PROJECT ID 8120-00-60					6,500	
0010	126+00	-	915+75	STH 13	12,400	YELLOW
0010	126+00	-	915+75	STH 13	6,200	WHITE
TOTAL PROJECT ID 8510-01-60					18,600	
GRAND TOTAL					55,800	

CHIP/SCRUB/FOG SEAL

CAT	STATION	TO	STATION	LOCATION	SPV.0180.01	SPV.0180.02	SPV.0180.03	REMARKS
					CHIP SEAL	SCRUB SEAL	FOG SEAL	
0010	133+00	-	433+00	USH 2	--	104,800	120,800	
TOTAL PROJECT ID 1180-00-63					--	104,800	120,800	
0010	112+25	-	401+25	STH 65	83,200	--	99,100	
TOTAL PROJECT ID 1540-00-60					83,200	--	99,100	
0010	109+50	-	232+00	USH 63	53,300	--	56,400	
TOTAL PROJECT ID 1550-01-60					53,300	--	56,400	
0010	113+00	-	372+40	STH 46	--	73,500	87,800	
TOTAL PROJECT ID 8070-00-60					--	73,500	87,800	
0010	122+00	-	325+75	STH 48	--	57,800	69,200	
TOTAL PROJECT ID 8120-00-60					--	57,800	69,200	
0010	126+00	-	915+75	STH 13	--	221,900	274,200	
TOTAL PROJECT ID 8510-01-60					--	221,900	274,200	
GRAND TOTAL					136,500	458,000	707,500	

USH 2 PERMANENT SIGNING TYPE II													
CATEGORY	SIGN #	SIGN CODE	SIGN SIZE			637.2210	637.2230	634.0614	634.0616	634.0618	638.2602	638.3000	REMARKS
			W IN	X	H IN	SIGNS TYPE II REFLECTIVE	SIGNS TYPE II REFLECTIVE	WOOD POST 4X6-INCH			REMOVING	REMOVING	
						H SF	F SF	14 FT EACH	16 FT EACH	18 FT EACH	SIGNS TYPE II EACH	SMALL SIGN SUPPORTS EACH	SIGN MOUNTED ON SAME POST AS
0010													
	1-01	D1-1		ESTR		---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	1-02	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	1-03	I55-56	30	X	36	7.50	---	---	1	---	1	1	ADOPT A HIGHWAY SPONSOR
	1-04	R3-55R	24	X	30	5.00	---	1	---	---	1	1	RIGHT TURN LANE W/DOWN LEFT ARROW
	1-05	W11-6	30	X	30	---	6.25	---	1	---	1	1	SNOWMOBILE CROSSING SYMBOL
	1-06	D1-1		ESTR		---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	1-07	R4-51-A	36	X	48	12.00	---	---	1	---	---	---	PASSING LANE AHEAD 2 MILES
	1-08	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	2-01	D1-1	90	X	15	9.38	---	2	---	---	1	2	ONE DESTINATION (ARROW)
	2-02	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	2-03	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	3-01	D1-1	90	X	15	9.38	---	2	---	---	1	2	ONE DESTINATION (ARROW)
	3-02	D1-1		ESTR		---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	3-03	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	4-01	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	4-02	D1-1		ESTR		---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	4-03	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	4-04	I55-56	30	X	36	7.50	---	---	1	---	1	1	ADOPT A HIGHWAY SPONSOR
	4-05	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	4-06	R4-51	36	X	48	12.00	---	---	1	---	---	---	PASSING LANE AHEAD 1/2 MILE
	5-01	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	5-02	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	5-03	R4-3	24	X	30	5.00	---	1	---	---	1	1	SLOWER TRAFFIC KEEP RIGHT
	5-04	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 55 MPH
	5-05	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 55 MPH
	6-01	D1-1		ESTR		---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	6-02	I55-56	30	X	36	7.50	---	---	1	---	1	1	ADOPT A HIGHWAY SPONSOR
	6-03	R4-53	54	X	54	20.25	---	---	---	2	1	1	DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE
	6-04	I55-56	30	X	36	7.50	---	---	1	---	1	1	ADOPT A HIGHWAY SPONSOR
	6-05	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	6-06	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	6-07	D1-1		ESTR		---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	6-08	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	6-09	R4-1	24	X	30	5.00	---	1	---	---	---	---	DO NOT PASS
	7-01	R4-53	54	X	54	20.25	---	---	---	2	1	1	DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE
	7-02	D2-2		ESTR		---	---	---	---	---	---	---	DESTINATION/DISTANCE (TWO DESTINATION)
	7-03	W9-1R	36	X	36	---	9.00	---	2	---	1	1	RIGHT LANE ENDS
	7-04	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	7-05	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	7-06	W4-2R	36	X	36	---	9.00	---	2	---	1	1	LANE REDUCTION TRANSITION SYMBOL (RIGHT)
	7-07	W4-2R	36	X	36	---	9.00	---	2	---	1	1	LANE REDUCTION TRANSITION SYMBOL (RIGHT)
	7-08	D1-2	96	X	30	20.00	---	2	---	---	1	2	TWO DESTINATION (ARROWS)
	7-09	R4-53	54	X	54	20.25	---	---	---	2	1	2	DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE
	8-01	D1-1		ESTR		---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	8-02	W14-3	48	X	36	---	6.00	---	1	---	1	1	NO PASSING ZONE
	8-03	W4-2R	36	X	36	---	9.00	---	2	---	1	1	LANE REDUCTION TRANSITION SYMBOL (RIGHT)
	8-04	W4-2R	36	X	36	---	9.00	---	2	---	1	1	LANE REDUCTION TRANSITION SYMBOL (RIGHT)
	8-05	R3-72	30	X	36	7.50	---	---	1	---	1	1	ONCOMING TRAFFIC USES CENTER LANE
	8-06	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	8-07	R4-53	54	X	54	20.25	---	---	---	2	---	---	DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE
	SUBTOTAL					252.87	99.25	20	26	8	38	42	

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USH 2 PERMANENT SIGNING TYPE II (CONTINUED)														
CATEGORY	SIGN #	SIGN CODE	W IN	SIGN SIZE		637.2210	637.2230	634.0614	634.0616	634.0618	638.2602	638.3000	REMARKS	
				X IN	H IN	SIGNS	SIGNS	WOOD POST 4X6-INCH			REMOVING	REMOVING		SIGN
						TYPE II	TYPE II				SIGNS	SMALL SIGN		MOUNTED
						REFLECTIVE	REFLECTIVE	14 FT	16 FT	18 FT	TYPE II	SUPPORTS		ON SAME
					H SF	F SF	EACH	EACH	EACH	EACH	EACH	POST AS		
0010	SUBTOTAL					252.87	99.25	20	26	8	38	42		
	8-08	W9-1R	36	X	36	---	9.00	---	2	---	1	1	---	RIGHT LANE ENDS
	8-09	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	9-01	W11-6	30	X	30	---	6.25	---	1	---	1	1	---	SNOWMOBILE CROSSING SYMBOL
	9-02	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	9-03	R4-1	24	X	30	5.00	---	1	---	---	---	---	---	DO NOT PASS
	9-04	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	9-05	R3-72	30	X	36	7.50	---	---	1	---	1	1	---	ONCOMING TRAFFIC USES CENTER LANE
	9-06	W11-6	30	X	30	---	6.25	---	1	---	1	1	---	SNOWMOBILE CROSSING SYMBOL
	9-07	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	9-08	I55-56	30	X	36	7.50	---	---	1	---	1	1	---	ADOPT A HIGHWAY SPONSOR
	9-09	R4-53	54	X	54	20.25	---	---	---	2	---	---	---	DO NOT PASS WHEN ONCOMING TRAFFIC IN EITHER LANE
	9-10	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	10-01	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	10-02	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	10-03	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	10-04	R4-3	24	X	30	5.00	---	1	---	---	1	1	---	SLOWER TRAFFIC KEEP RIGHT
	10-05	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	10-06	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	10-07	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	10-08	W3-5	36	X	36	---	9.00	---	2	---	1	1	---	SPEED LIMIT 25 AHEAD (ARROW)
	10-09	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	10-10	I3-1	36	X	24	6.00	---	---	---	---	1	---	10-11	LAKE OR RIVER NAME
	10-11	R9-56	18	X	24	3.00	---	---	1	---	1	1	10-10	NO FISHING OR DIVING FROM BRIDGE
	10-12	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	10-13	I3-1	36	X	24	6.00	---	---	---	---	1	---	10-14	LAKE OR RIVER NAME
	10-14	R9-56	18	X	24	3.00	---	---	1	---	1	1	10-13	NO FISHING OR DIVING FROM BRIDGE
	10-15	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	10-16	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	10-17	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	10-18	J1-1	24	X	39	6.50	---	---	1	---	1	1	---	JUNCTION OR END ASSEMBLY
	10-19	J4-1	24	X	36	6.00	---	---	1	---	1	---	---	REASSURANCE ASSEMBLY
	10-20	D2-2	72	X	24	12.00	---	---	2	---	1	2	---	DESTINATION/DISTANCE (TWO DESTINATION)
	10-21	I2-3		ESTR		---	---	---	---	---	---	---	---	IRON RIVER UNINCORPORATED
	10-22	R59-51	36	X	36	9.00	---	---	1	---	1	1	---	STOP FOR BUS FLASHING LIGHTS
	10-23	R2-1	24	X	30	5.00	---	1	---	---	1	1	---	SPPED LIMIT 25 MPH
	10-24	R2-1	24	X	30	5.00	---	1	---	---	1	1	---	SPEED LIMIT 55 MPH
	10-25	D1-1		ESTR		---	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)
	10-26	R4-51	36	X	48	12.00	---	---	1	---	---	1	---	PASSING LANE AHEAD 1/2 MILE
	11-01	W11-2		REMOVE		---	---	---	---	---	1	1	---	PEDESTRIAN CROSSING SYMBOL
	11-02	J13-1	24	X	45	7.50	---	---	---	---	1	---	11-03	DIRECTIONAL WITHOUT CARDINAL
	11-03	D1-1		ESTR		---	---	---	---	---	---	---	11-02	ONE DESTINATION (ARROW)
	11-04	J13-1	24	X	45	7.50	---	---	1	---	1	1	---	DIRECTIONAL WITHOUT CARDINAL
	11-05	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	11-06	R1-1	36	X	36	7.46	---	---	1	---	1	1	---	STOP SIGN
PROJECT 1180-00-63 TOTAL						414.80	153.75	32	46	10	69	71		

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STH 65 PERMANENT SIGNING TYPE II													
CATEGORY	SIGN #	SIGN CODE	W IN	SIGN SIZE		637.2210	637.2230	634.0614	634.0616	638.2602	638.3000	REMARKS	
				X IN	H IN	SIGNS	SIGNS	POSTS WOOD 4X6-INCH	REMOVING	REMOVING	SIGN		
						TYPE II	TYPE II						
						REFLECTIVE	REFLECTIVE						
						H SF	F SF						14 FT EACH
0010													
	1-01	J4-1		ESTR	---	---	---	---	---	---	---	REASSURANCE ASSEMBLY	
	1-02	R5-1A		ESTR	---	---	---	---	---	---	---	WRONG WAY	
	1-03	I55-56		ESTR	---	---	---	---	---	---	---	ADOPT A HIGHWAY SPONSOR	
	1-04	R5-1A		ESTR	---	---	---	---	---	---	---	WRONG WAY	
	1-05	R2-1		ESTR	---	---	---	---	---	---	---	SPEED LIMIT 25 MPH	
	1-06	I2-3		ESTR	---	---	---	---	---	---	---	ROBERTS POPULATION 1651	
	1-07	J1-1		ESTR	---	---	---	---	---	---	---	JUNCTION OR END ASSEMBLY	
	1-08	W14-3		ESTR	---	---	---	---	---	---	---	NO PASSING ZONE	
	1-09	D1-1		ESTR	---	---	---	---	---	---	---	ONE DESTINATION (ARROW)	
	2-01	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	2-02	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	4-01	I55-56	30	X	36	7.50	---	---	1	1	1	ADOPT A HIGHWAY SPONSOR	
	4-02	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	4-03	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	4-04	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	5-01	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	5-02	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	6-01	J1-1	24	X	39	6.50	---	---	1	1	1	JUNCTION OR END ASSEMBLY	
	6-02	I55-56	30	X	36	7.50	---	---	1	1	1	ADOPT A HIGHWAY SPONSOR	
	6-03	J4-1	24	X	36	6.00	---	---	1	1	1	REASSURANCE ASSEMBLY	
	6-04	J13-1	24	X	45	7.50	---	---	1	1	1	DIRECTIONAL WITHOUT CARDINAL	
	6-05	J13-1	24	X	45	7.50	---	---	1	1	1	DIRECTIONAL WITHOUT CARDINAL	
	6-06	R1-1	36	X	36	7.46	---	---	1	1	1	STOP SIGN	
	6-07	R1-1	36	X	36	7.46	---	---	1	1	1	STOP SIGN	
	6-08	J13-1	24	X	45	7.50	---	---	1	1	1	DIRECTIONAL WITHOUT CARDINAL	
	6-09	J13-1	24	X	45	7.50	---	---	1	1	1	DIRECTIONAL WITHOUT CARDINAL	
	6-10	J4-1	24	X	36	6.00	---	---	1	1	1	REASSURANCE ASSEMBLY	
	6-11	I55-56	30	X	36	7.50	---	---	1	1	1	6-12 ADOPT A HIGHWAY SPONSOR	
	6-12	W14-3	48	X	36	---	6.00	---	---	1	---	6-11 NO PASSING ZONE	
	6-13	J1-1	24	X	39	6.50	---	---	1	1	1	JUNCTION OR END ASSEMBLY	
	8-01	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	8-02	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	8-03	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	10-01	I55-56	30	X	36	7.50	---	---	1	1	1	ADOPT A HIGHWAY SPONSOR	
	10-02	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	10-03	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	10-04	I55-56	30	X	36	7.50	---	---	1	1	1	ADOPT A HIGHWAY SPONSOR	
	10-05	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	11-01	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	11-02	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	11-03	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	11-04	J1-1		ESTR		---	---	---	---	---	---	JUNCTION OR END ASSEMBLY	
PROJECT 1540-00-60 TOTAL						159.22	42.00	10	21	32	31		

USH 63 PERMANENT SIGNING TYPE II													
CATEGORY	SIGN #	SIGN CODE	W IN	SIGN SIZE		637.2210	637.2230	634.0614	634.0616	634.0618	638.2602	638.3000	REMARKS
				X	H	SIGNS	SIGNS	POSTS WOOD 4X6-INCH			REMOVING	REMOVING	
				IN	IN	TYPE II	TYPE II				SIGNS	SMALL SIGN	
						REFLECTIVE	REFLECTIVE	14 FT	16 FT	18 FT	TYPE II	SUPPORTS	SIGN MOUNTED ON SAME
0010						H SF	F SF	EACH	EACH	EACH	EACH	EACH	POST AS
	1-01	J4-1		ESTR		---	---	---	---	---	---	---	1-02
	1-02	D2-1		ESTR		---	---	---	---	---	---	---	1-01
	1-03	R2-1		ESTR		---	---	---	---	---	---	---	
	1-04	R5-1		ESTR		---	---	---	---	---	---	---	
	1-05	R1-1		ESTR		---	---	---	---	---	---	---	
	1-06	J3-3		ESTR		---	---	---	---	---	---	---	
	1-07	R4-7		ESTR		---	---	---	---	---	---	---	
	1-08	W1-7		ESTR		---	---	---	---	---	---	---	
	1-09	JH-1		ESTR		---	---	---	---	---	---	---	
	1-10	R2-1	24	X	30	5.00	---	1	---	---	1	1	
	1-11	J4-2	48	X	36	12.00	---	---	1	---	1	1	
	1-12	J3-2	48	X	57	19.00	---	---	---	1	1	1	
	1-13	D1-2	84	X	30	17.50	---	2	---	---	1	2	
	1-14	R3-9B	24	X	36	6.00	---	---	1	---	1	1	
	1-15	J2-2	48	X	57	19.00	---	---	---	1	1	1	
	1-16	R2-1	24	X	30	5.00	---	1	---	---	1	1	
	2-01	R2-1	24	X	30	5.00	---	1	---	---	1	1	
	2-02	R1-1	30	X	30	5.18	---	1	---	---	1	1	
	2-03	R7-66		ESTR		---	---	---	---	---	---	---	
	2-04	S1-1	36	X	36	---	6.75	---	---	1	1	1	2-05, 2-06
	2-05	S16-9P	30	X	18	---	3.75	---	---	---	1	---	2-04, 2-06
	2-06	R2-6P	24	X	18	---	3.00	---	---	---	1	---	2-04, 2-05
	2-07	E10-82		ESTR		---	---	---	---	---	---	---	
	2-08	D2-18		ESTR		---	---	---	---	---	---	---	
	2-09	W14-3	48	X	36	---	6.00	---	1	---	1	1	2-10
	2-10	R2-1	24	X	30	5.00	---	---	---	---	1	---	2-09
	2-11	R2-1	24	X	30	5.00	---	1	---	---	1	1	
	2-12	S5-2	24	X	30	5.00	---	1	---	---	1	1	
	2-13	S1-1	36	X	36	---	6.75	---	1	---	1	1	2-14
	2-14	S16-7L	30	X	18	---	3.75	---	---	---	1	---	2-13
	2-15	R1-1	30	X	30	5.18	---	1	---	---	1	1	
	2-16	R1-1	30	X	30	5.18	---	1	---	---	1	1	
	2-17	S1-1	36	X	36	---	6.75	---	1	---	1	1	2-18
	2-18	S16-7L	30	X	18	---	3.75	---	---	---	1	---	2-17
	2-19	W54-59		REMOVE		---	---	---	---	---	1	1	
	2-20	R3-9B	24	X	36	6.00	---	---	1	---	1	1	
	2-21	JH-1	24	X	45	7.50	---	---	1	---	1	1	
	2-22	R3-9B	24	X	36	6.00	---	---	1	---	1	1	
	2-23	S5-2	24	X	30	5.00	---	1	---	---	1	1	
	2-24	R1-1	30	X	30	5.18	---	1	---	---	1	1	
	2-25	S1-1	36	X	36	---	6.75	---	---	---	1	---	POLE, 2-26, 2-27
	2-26	S16-9P	30	X	18	---	3.75	---	---	---	1	---	POLE, 2-25, 2-27
	2-27	R2-6P	24	X	18	---	3.00	---	---	---	1	---	POLE, 2-25, 2-26
	2-28	W54-59		REMOVE		---	---	---	---	---	1	1	
	2-29	JH-1	24	X	45	7.50	---	---	1	---	1	1	
	2-30	R1-1	30	X	30	5.18	---	1	---	---	1	1	
	2-31	R1-1	30	X	30	5.18	---	1	---	---	1	1	
	2-32	R3-9B	24	X	36	6.00	---	---	---	---	1	---	POLE
	2-33	R2-1	24	X	30	5.00	---	1	---	---	1	1	
	2-34	R1-1	30	X	30	5.18	---	1	---	---	1	1	
	SUBTOTAL					182.76	54.00	16	9	3	38	30	

USH 63 PERMANENT SIGNING TYPE II (CONTINUED)														
CATEGORY	SIGN #	SIGN CODE	SIGN SIZE			637.2210 SIGN TYPE II REFLECTIVE	637.2230 SIGN TYPE II REFLECTIVE	634.0614 634.0616 634.0618 POSTS WOOD 4X6-INCH			638.2602 REMOVING SIGN TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	SIGN MOUNTED ON SAME POST AS	REMARKS
			W IN	X	H IN	H SF	F SF	14 FT EACH	16 FT EACH	18 FT EACH				
0010	SUBTOTAL					182.76	54.00	16	9	3	38	30		
	2-35	R2-1	24	X	30	5.00	---	1	---	---	1	1	---	SPEED LIMIT 25 MPH
	2-36	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	2-37	J2-2	48	X	57	19.00	---	---	---	1	1	1	---	ROUTE TURN ASSEMBLY
	3-01	J4-2	48	X	36	12.00	---	---	1	---	1	1	---	REASSURANCE ASSEMBLY
	3-02	W3-1	36	X	36	---	9.00	---	2	---	1	1	---	STOP AHEAD
	3-03	R2-1	24	X	30	5.00	---	1	---	---	1	1	---	SPEED LIMIT 25 MPH
	3-04	D1-2	72	X	30	15.00	---	2	---	---	1	1	---	TWO DESTINATION (ARROWS)
	3-05	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	3-06	R1-1	36	X	36	7.46	---	---	---	---	1	---	POLE, 3-07	STOP SIGN
	3-07	R1-3P	24	X	9	1.50	---	---	---	---	1	---	POLE, 3-06	ALL WAY
	3-08	J2-1	24	X	57	9.50	---	---	---	---	1	---	POLE	ROUTE TURN ASSEMBLY
	3-09	JH-1	24	X	45	7.50	---	---	1	---	1	1	---	HOSPITAL ROUTE ASSEMBLY
	3-10	J13-2	48	X	45	15.00	---	---	---	---	1	---	POLE	DIRECTIONAL WITHOUT CARDINAL
	3-11	R1-1	36	X	36	7.46	---	---	---	---	1	---	POLE, 3-12	STOP SIGN
	3-12	R1-3P	24	X	9	1.50	---	---	---	---	1	---	POLE, 3-11	ALL WAY
	3-13	R1-1	36	X	36	7.46	---	---	---	---	1	---	POLE, 3-14	STOP SIGN
	3-14	R1-3P	24	X	9	1.50	---	---	---	---	1	---	POLE, 3-13	ALL WAY
	3-15	J12-2	48	X	45	15.00	---	---	1	---	1	1	---	ROUTE TURN WITHOUT CARDINAL
	3-16	R1-1	36	X	36	7.46	---	---	---	---	1	---	POLE, 3-17	STOP SIGN
	3-17	R1-3P	24	X	9	1.50	---	---	---	---	1	---	POLE, 3-16	ALL WAY
	3-18	R7-5D		ESTR		---	---	---	---	---	---	---	---	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	3-19	R2-1	24	X	30	5.00	---	---	---	---	1	---	POLE	SPEED LIMIT 25 MPH
	3-20	W3-1	36	X	36	---	9.00	---	---	---	1	---	POLE	STOP AHEAD
	3-21	R7-5D		ESTR		---	---	---	---	---	---	---	---	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	3-22	J1-1	24	X	39	6.50	---	---	---	---	1	---	POLE	JUNCTION OR END ASSEMBLY
	3-23	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	4-01	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	4-02	R7-5D		ESTR		---	---	---	---	---	---	---	POLE, 4-03	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-03	R2-1	24	X	30	5.00	---	---	---	---	1	---	POLE, 4-02	SPEED LIMIT 25 MPH
	4-04	R7-5D		ESTR		---	---	---	---	---	---	---	POLE, 4-05	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-05	R2-1	24	X	30	5.00	---	---	---	---	1	---	POLE, 4-04	SPEED LIMIT 25 MPH
	4-06	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	4-07	R7-5D		ESTR		---	---	---	---	---	1	---	POLE, 4-08	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-08	R3-4	24	X	24	4.00	---	---	---	---	1	---	POLE, 4-07	NO U TURN SYMBOL
	4-09	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	4-10	S4-52		ESTR		---	---	---	---	---	---	---	ROAD MOUNT	SCHOOL YIELD TO PEDESTRIANS WITHIN CROSSWALK
	4-11	S4-52		ESTR		---	---	---	---	---	---	---	ROAD MOUNT	SCHOOL YIELD TO PEDESTRIANS WITHIN CROSSWALK
	4-12	R7-5D		ESTR		---	---	---	---	---	---	---	POLE, 4-13	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-13	S5-2	24	X	30	5.00	---	---	---	---	1	---	POLE, 4-12	END SCHOOL ZONE
	4-14	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	4-15	R7-5D		ESTR		---	---	---	---	---	---	---	POLE	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-16	S1-1	36	X	36	---	6.75	---	---	---	1	---	POLE, 4-17, 4-18	SCHOOL ADVANCE
	4-17	S16-9P	30	X	18	---	3.75	---	---	---	1	---	POLE, 4-16, 4-18	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)
	4-18	R2-6P	24	X	18	---	3.00	---	---	---	1	---	POLE, 4-16, 4-17	FINES HIGHER PLAQUE
	4-19	R7-5D		ESTR		---	---	---	---	---	---	---	POLE, 4-20	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-20	R2-1	24	X	30	5.00	---	---	---	---	1	---	POLE, 4-19	SPEED LIMIT 25 MPH
	4-21	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	4-22	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	4-23	S4-51	24	X	48	8.00	---	---	---	---	1	---	POLE	SCHOOL SPEED LIMIT
	4-24	R7-5D		ESTR		---	---	---	---	---	---	---	---	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	SUBTOTAL					411.72	85.50	29	14	4	78	47		

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USH 63 PERMANENT SIGNING TYPE II (CONTINUED)													
CATEGORY	SIGN #	SIGN CODE	SIGN SIZE			637.2210	637.2230	634.0614	634.0616	634.0618	638.2602	638.3000	REMARKS
			W	X	H	SIGNS	SIGNS	POSTS WOOD 4X6-INCH			REMOVING	REMOVING	
			IN	IN	IN	TYPE II	TYPE II				SIGNS	SMALL SIGN	
						REFLECTIVE	REFLECTIVE	14 FT	16 FT	18 FT	TYPE II	SUPPORTS	SIGN MOUNTED ON SAME POST AS
						H	F	EACH	EACH	EACH	EACH	EACH	
0010	SUBTOTAL					411.72	85.50	29	14	4	78	47	
	4-25	R7-5D		ESTR		---	---	---	---	---	---	---	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-26	S1-1	36	X	36	---	6.75	---	---	---	1	---	SCHOOL ADVANCE
	4-27	S16-7L	30	X	18	---	3.75	---	---	---	1	---	LEFT DIAGONAL DOWNWARD ARROW (FLUORESCENT YELLOW GREEN)
	4-28	S1-1	36	X	36	---	6.75	---	---	---	1	---	SCHOOL ADVANCE
	4-29	S16-7L	30	X	18	---	3.75	---	---	---	1	---	LEFT DIAGONAL DOWNWARD ARROW (FLUORESCENT YELLOW GREEN)
	4-30	S4-52		ESTR		---	---	---	---	---	---	---	SCHOOL YIELD TO PEDESTRIANS WITHIN CROSSWALK
	4-31	S4-52		ESTR		---	---	---	---	---	---	---	SCHOOL YIELD TO PEDESTRIANS WITHIN CROSSWALK
	4-32	W1-1R	36	X	36	---	9.00	---	---	---	1	---	RIGHT TURN
	4-33	W13-1	24	X	24	---	4.00	---	---	---	1	---	ADVISORY SPEED (YELLOW BACK)
	4-34	R7-5D		ESTR		---	---	---	---	---	---	---	2 HOUR PARKING 9AM TO 5PM (DOUBLE ARROW)
	4-35	S4-51	24	X	48	8.00	---	---	---	---	1	---	SCHOOL SPEED LIMIT
	4-36	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	4-37	S1-1	36	X	36	---	6.75	---	---	---	1	---	SCHOOL ADVANCE
	4-38	S16-9P	30	X	18	---	3.75	---	---	---	1	---	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)
	4-39	R2-6P	24	X	18	---	3.00	---	---	---	1	---	FINES HIGHER PLAQUE
	4-40	W1-6	48	X	24	---	8.00	1	---	---	1	1	NIGHT ARROW (SINGLE)
	4-41	W1-6	48	X	24	---	8.00	---	---	---	1	---	NIGHT ARROW (SINGLE)
	4-42	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	4-43	S5-2	24	X	30	5.00	---	---	---	---	1	---	END SCHOOL ZONE
	4-44	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 25 MPH
	4-45	R2-1	24	X	30	5.00	---	---	---	---	1	---	SPEED LIMIT 25 MPH
	4-46	W1-1L	36	X	36	---	9.00	---	---	---	1	---	LEFT TURN
	4-47	W13-1	24	X	24	---	4.00	---	---	---	1	---	ADVISORY SPEED (YELLOW BACK)
	5-01	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 25 MPH
	5-02	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 25 MPH
	5-03	D11-6		ESTR		---	---	---	---	---	---	---	5-02 SNOWMOBILE ROUTE W/SYMBOL
	5-04	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	5-05	R2-1	24	X	30	5.00	---	---	---	---	1	---	SPEED LIMIT 25 MPH
	5-06	R2-1	24	X	30	5.00	---	---	---	---	1	---	SPEED LIMIT 40 MPH
	5-07	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	5-08	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	5-09	R1-1	30	X	30	5.18	---	---	---	---	1	---	STOP SIGN
	5-10	W3-5	36	X	36	---	9.00	---	---	---	1	---	SPEED LIMIT 25 AHEAD (ARROW)
	5-11	D11-6		ESTR		---	---	---	---	---	---	---	SNOWMOBILE ROUTE W/SYMBOL
	5-12	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	5-13	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 40 MPH
	5-14	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	5-15	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	5-16	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 40 MPH
	5-17	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 40 MPH
	5-18	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	6-01	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 40 MPH
	6-02	R1-1	30	X	30	5.18	---	1	---	---	1	1	STOP SIGN
	6-03	R2-1	24	X	30	5.00	---	1	---	---	1	1	SPEED LIMIT 55 MPH
PROJECT 1550-01-60 TOTAL						536.70	171.00	48	14	4	116	66	

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STH 46 PERMANENT SIGNING TYPE II													
CATEGORY	SIGN #	SIGN CODE	SIGN SIZE			637.2210	637.2230	634.0614	634.0616	638.2602	638.3000	REMARKS	
			W IN	X	H IN	SIGNS	SIGNS	POSTS WOOD 4X6-INCH		REMOVING	REMOVING		SIGN
						TYPE II	TYPE II	14 FT	16 FT	TYPE II	SMALL SIGN		MOUNTED
						REFLECTIVE	REFLECTIVE			SUPPORTS	ON SAME		
						REFLECTIVE	REFLECTIVE			TYPE II	SUPPORTS		ON SAME
0010													
	1-01	R7-66		ESTR		---	---	---	---	---	---	NO PARKING ON ANY CITY STREET 2AM TO 8AM	
	1-02	R1-1		ESTR		---	---	---	---	---	---	STOP SIGN	
	1-03	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	1-04	R2-1	24	X	30	5.00	---	---	1	1	1	SPEED LIMIT 40 MPH	
	1-05	R7-1L	18	X	24	3.00	---	1	---	1	---	NO PARKING ANY TIME (LEFT ARROW)	
	1-06	R2-1	24	X	30	5.00	---	---	1	1	1	SPEED LIMIT 55 MPH	
	1-07	R7-1R	18	X	24	3.00	---	1	---	1	---	NO PARKING ANY TIME (RIGHT ARROW)	
	1-08	I2-3	60	X	24	10.00	---	2	---	1	1	AMERY POPULATION 2092	
	1-09	I55-56	30	X	36	7.50	---	---	1	1	1	ADOPT A HIGHWAY SPONSOR	
	1-10	W3-5	36	X	36	---	9.00	---	2	1	1	SPPED LIMIT 40 AHEAD (ARROW)	
	1-11	R10-64	30	X	36	7.50	---	---	1	1	1	NO ENGINE BREAKING EXCEPT IN EMERGENCY	
	1-12	R10-64F	30	X	18	3.75	---	---	---	1	---	WITHIN CITY OR VILLAGE LIMITS	
	1-13	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	1-14	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	2-01	J1-1	24	X	39	6.50	---	---	1	1	1	JUNCTION OR END ASSEMBLY	
	2-02	E10-82		ESTR		---	---	---	---	---	---	TOURIST ORIENTED DIRECTIONAL SIGNS	
	2-03	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	2-04	J12-1	24	X	45	7.50	---	---	1	1	1	ROUTE TURN WITHOUT CARDINAL	
	2-05	R1-1	36	X	36	7.46	---	---	1	1	1	STOP SIGN	
	2-06	J12-1	24	X	45	7.50	---	---	1	1	1	ROUTE TURN WITHOUT CARDINAL	
	2-07	R3-55R	24	X	30	5.00	---	1	---	1	1	RIGHT TURN LANE W/DOWN LEFT ARROW	
	2-08	M1-6		REMOVE		---	---	---	---	1	1	STATE ROUTE MARKER (2-3 DIGITS)	
	2-09	W1-7	48	X	24	---	8.00	---	---	1	---	NIGHT ARROW (DOUBLE)	
	2-10	E10-82		ESTR		---	---	---	---	---	---	TOURIST ORIENTED DIRECTIONAL SIGNS	
	2-11	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	2-12	J1-1	24	X	39	6.50	---	---	1	1	1	JUNCTION OR END ASSEMBLY	
	2-13	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	2-14	J4-1	24	X	36	6.00	---	---	1	---	---	REASSURANCE ASSEMBLY	
	2-15	J4-1	24	X	36	6.00	---	---	1	---	---	REASSURANCE ASSEMBLY	
	2-16	J12-1	24	X	45	7.50	---	---	1	---	---	ROUTE TURN WITHOUT CARDINAL	
	3-01	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	3-02	W14-3	48	X	36	---	---	---	1	1	1	NO PASSING ZONE	
	3-03	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	4-01	E10-82		ESTR		---	---	---	---	---	---	TOURIST ORIENTED DIRECTIONAL SIGNS	
	4-02	I55-56	30	X	36	7.50	---	---	1	1	1	ADOPT A HIGHWAY SPONSOR	
	4-03	I55-56	30	X	36	7.50	---	---	1	1	1	ADOPT A HIGHWAY SPONSOR	
	4-04	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	4-05	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	5-01	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	5-02	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	6-01	R1-1	30	X	30	5.18	---	1	---	1	1	STOP SIGN	
	6-02	R7-1L	24	X	30	5.00	---	1	---	1	1	NO PARKING ANY TIME (LEFT ARROW)	
	6-03	W14-3	48	X	36	---	6.00	---	1	1	1	NO PASSING ZONE	
	6-04	R7-1R	24	X	30	5.00	---	1	---	1	1	NO PARKING ANY TIME (RIGHT ARROW)	
	6-05	R7-1D	24	X	30	5.00	---	1	---	1	1	NO PARKING ANY TIME (DOUBLE ARROW)	
	6-06	R7-1D	24	X	30	5.00	---	1	---	1	1	NO PARKING ANY TIME (DOUBLE ARROW)	
	6-07	R7-1D	24	X	30	5.00	---	1	---	1	1	NO PARKING ANY TIME (DOUBLE ARROW)	
	6-08	R7-1D	24	X	30	5.00	---	1	---	1	1	NO PARKING ANY TIME (DOUBLE ARROW)	
	6-09	R7-1D	24	X	30	5.00	---	1	---	1	1	NO PARKING ANY TIME (DOUBLE ARROW)	
	6-10	I3-1	36	X	24	6.00	---	---	1	1	1	LAKE OR RIVER NAME	
	6-11	R9-56	18	X	24	3.00	---	---	---	1	---	NO FISHING OR DIVING FROM BRIDGE	
	6-12	I3-1	36	X	24	6.00	---	---	1	1	1	LAKE OR RIVER NAME	
	6-13	R9-56	18	X	24	3.00	---	---	---	1	---	NO FISHING OR DIVING FROM BRIDGE	
	SUBTOTAL					203.79	65.00	18	27	45	39		

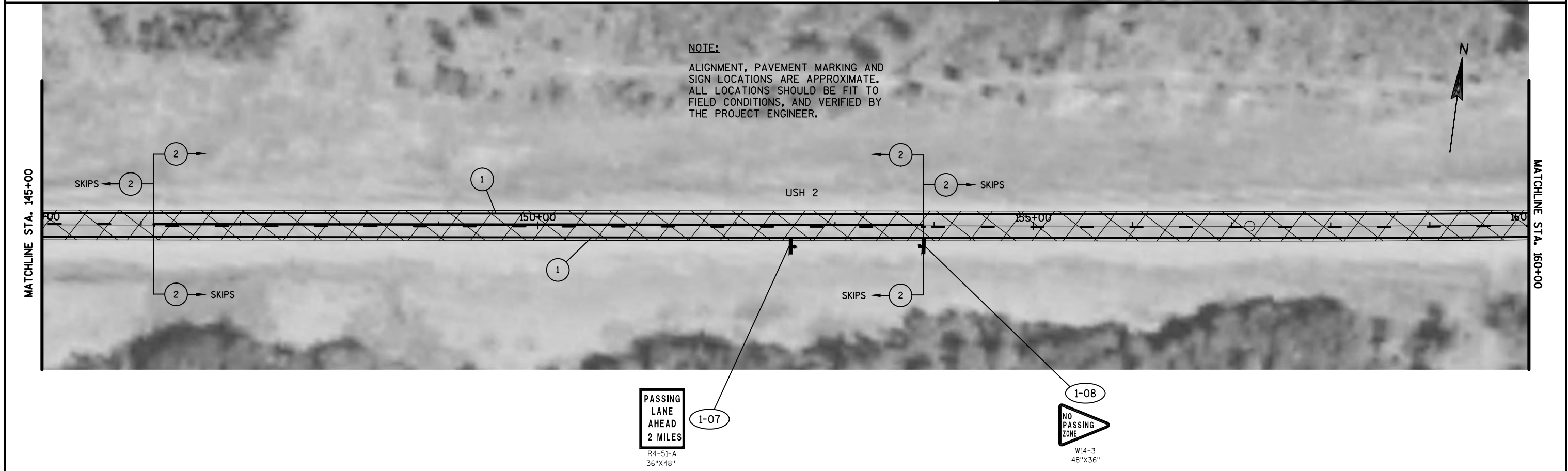
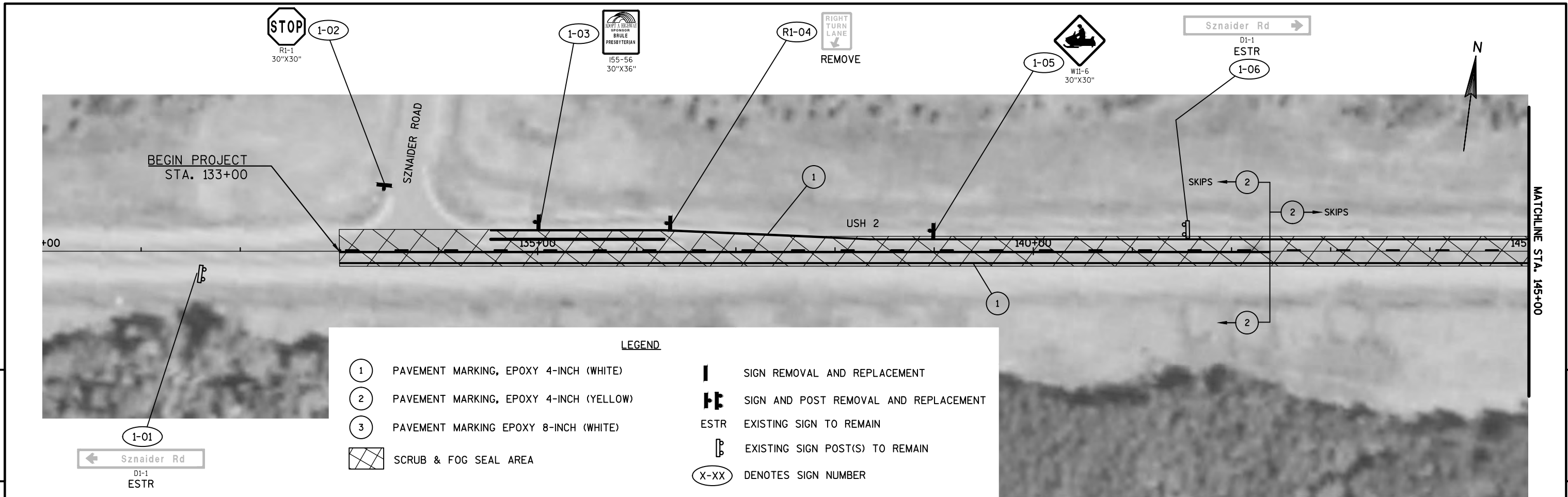
STH 46 PERMANENT SIGNING TYPE II (CONTINUED)													
CATEGORY	SIGN #	SIGN CODE	SIGN SIZE			637.2210	637.2230	634.0614	634.0616	638.2602	638.3000	SIGN MOUNTED ON SAME POST AS	REMARKS
			W IN	X IN	H IN	SIGNS	SIGNS			REMOVING	REMOVING		
						TYPE II	TYPE II	POSTS WOOD 4X6-INCH		SIGNS	SMALL SIGN		
						REFLECTIVE	REFLECTIVE	14 FT	16 FT	TYPE II	SUPPORTS		
					H SF	F SF	EACH	EACH	EACH	EACH			
0010	SUBTOTAL					203.79	65.00	18	27	45	39		
	6-14	R7-1D	24	X	30	5.00	---	1	---	1	1	---	NO PARKING ANY TIME (DOUBLE ARROW)
	6-15	R7-1D	24	X	30	5.00	---	1	---	1	1	---	NO PARKING ANY TIME (DOUBLE ARROW)
	6-16	R7-1D	24	X	30	5.00	---	1	---	1	1	---	NO PARKING ANY TIME (DOUBLE ARROW)
	7-01	R7-1D	24	X	30	5.00	---	1	---	1	1	---	NO PARKING ANY TIME (DOUBLE ARROW)
	7-02	R1-1	30	X	30	5.18	---	1	---	1	1	---	STOP SIGN
	7-03	R7-1R	24	X	30	5.00	---	1	---	1	1	---	NO PARKING ANY TIME (RIGHT ARROW)
	7-04	I55-56	30	X	36	7.50	---	---	1	1	1	---	ADOPT A HIGHWAY SPONSOR
	7-05	R7-1L	24	X	30	5.00	---	1	---	1	1	---	NO PARKING ANY TIME (LEFT ARROW)
	7-06	I55-56	30	X	36	7.50	---	---	1	1	1	---	ADOPT A HIGHWAY SPONSOR
	7-07	W14-3	48	X	36	---	6.00	---	1	1	1	---	NO PASSING ZONE
	7-08	R1-1	30	X	30	5.18	---	1	---	1	1	---	STOP SIGN
	7-09	W2-2	30	X	30	---	6.25	---	1	1	1	---	SIDE ROAD (90 DEGREES)
	8-01	D7-68	REMOVE			---	---	---	---	1	1	---	ROAD TO POLK COUNTY PARK 1 MILE
	8-02	W14-3	48	X	36	---	6.00	---	1	1	1	---	NO PASSING ZONE
	8-03	R1-1	30	X	30	5.18	---	1	---	1	1	---	STOP SIGN
	8-04	R1-1	30	X	30	5.18	---	1	---	1	1	---	STOP SIGN
	8-05	W14-3	48	X	36	---	6.00	---	1	1	1	---	NO PASSING ZONE
	9-01	MR2-1	REMOVE			---	---	---	---	1	1	9-02, 9-03	JCT RUSTIC ROAD (BROWN)
	9-02	MR1-99	REMOVE			---	---	---	---	---	---	9-01, 9-03	RUSTIC ROAD MARKER
	9-03	MR4-53	REMOVE			---	---	---	---	---	---	9-01, 9-02	RUSTIC ROAD ROUTE NUMBER PANEL
	9-04	W2-1	30	X	30	6.25	6.25	---	1	1	1	---	CROSS ROAD
	9-05	D7-68L	60	X	36	15.00	---	---	2	1	1	---	POLK COUNTY PARK (LEFT ARROW)
	9-06	DR1-53D	REMOVE			---	---	---	---	1	1	---	RUSTIC ROAD (DOUBLE ARROW)
	9-07	R1-1	30	X	30	5.18	---	1	---	1	1	---	STOP SIGN
	9-08	R1-1	30	X	30	5.18	---	1	---	1	1	---	STOP SIGN
	9-09	DR1-53D	REMOVE			---	---	---	---	1	1	---	RUSTIC ROAD (DOUBLE ARROW)
	9-10	D7-68R	60	X	36	15.00	---	---	2	1	1	---	POLK COUNTY PARK RIGHT ARROW)
	9-11	J13-1	24	X	45	7.50	---	---	1	---	---	---	POLK COUNTY PARK RIGHT ARROW)
	9-12	J13-1	24	X	45	7.50	---	---	1	---	---	---	POLK COUNTY PARK RIGHT ARROW)
	10-01	MR2-1	REMOVE			---	---	---	---	1	1	10-02,10-03	JCT RUSTIC ROAD (BROWN)
	10-02	MR1-99	REMOVE			---	---	---	---	---	---	10-01,10-03	RUSTIC ROAD MARKER
	10-03	MR4-53	REMOVE			---	---	---	---	---	---	10-01,10-02	RUSTIC ROAD ROUTE NUMBER PANEL
	10-04	J1-1	ESTR			---	---	---	---	---	---	---	JUNCTION OR END ASSEMBLY
	10-05	W2-6	ESTR			---	---	---	---	---	---	---	CIRCULAR INTERSECTION SIGN
	10-06	D1-3	ESTR			---	---	---	---	---	---	---	THREE DESTINATION (ARROWS)
	10-07	W3-2	ESTR			---	---	---	---	---	---	---	YIELD AHEAD
	10-08	D1-3	ESTR			---	---	---	---	---	---	---	THREE DESTINATION (ARROWS)
	10-09	I55-56	ESTR			---	---	---	---	---	---	---	ADOPT A HIGHWAY SPONSOR
PROJECT 8070-00-60 TOTAL						331.12	95.50	30	40	71	65		

STH 48 PERMANENT SIGNING TYPE II														
CATEGORY	SIGN #	SIGN CODE	SIGN SIZE			637.2210	637.2230	634.0614	634.0616	634.0618	638.2602	638.3000	SIGN MOUNTED ON SAME POST AS	REMARKS
			W IN	X IN	H IN	REFLECTIVE H SF	REFLECTIVE F SF	POSTS WOOD 4X6-INCH			REMOVING SIGNS	REMOVING SMALL SIGN		
								14 FT EACH	16 FT EACH	18 FT EACH	TYPE II EACH	SUPPORTS EACH		
0010														
	1-01	J4-1	24	X	36	6.00	---	---	1	---	1	---	---	REASSURANCE ASSEMBLY
	1-02	D2-1	90	X	15	9.38	---	---	2	---	1	2	---	DESTINATION/DISTANCE (ONE DESTINATION)
	1-03	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	1-04	J3-1	24	X	57	9.50	---	---	---	1	1	1	---	DIRECTIONAL ASSEMBLY
	1-05	J3-1	24	X	57	9.50	---	---	---	1	1	1	---	DIRECTIONAL ASSEMBLY
	1-06	D2-1	72	X	15	7.50	---	---	2	---	1	2	---	DESTINATION/DISTANCE (ONE DESTINATION)
	1-07	J4-1	24	X	36	6.00	---	---	1	---	1	---	---	REASSURANCE ASSEMBLY
	1-08	D1-2	84	X	30	17.50	---	2	---	---	1	2	---	TWO DESTINATION (ARROWS)
	1-09	J2-2	48	X	57	19.00	---	---	---	1	1	1	---	ROUTE TURN ASSEMBLY
	1-10	J1-1	30	X	36	7.50	---	---	1	---	1	1	---	JUNCTION OR END ASSEMBLY
	1-11	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	1-12	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	2-01	S3-1	36	X	36	---	9.00	---	2	---	1	1	---	SCHOOL BUS STOP AHEAD
	2-02	J1-1	24	X	39	6.50	---	---	1	---	1	1	---	JUNCTION OR END ASSEMBLY
	2-03	W5-2	36	X	36	---	9.00	---	2	---	1	1	---	NARROW BRIDGE WORD MESSAGE
	2-04	MR5-54L	REMOVE			---	---	---	---	---	1	1	---	TO RUSTIC ROAD [1] MILE/LEFT ARROW
	2-05	W54-58	30	X	30	---	6.25	---	1	---	1	1	---	CHURCH ENTRANCE
	2-06	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	2-07	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	2-08	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	2-09	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	3-01	J12-1	24	X	45	7.50	---	---	1	---	1	1	---	ROUTE TURN WITHOUT CARDINAL
	3-02	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	3-03	J13-1	24	X	45	7.50	---	---	1	---	1	1	---	DIRECTIONAL WITHOUT CARDINAL
	3-04	J13-1	24	X	45	7.50	---	---	1	---	1	1	---	DIRECTIONAL WITHOUT CARDINAL
	3-05	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	3-06	W5-2	36	X	36	---	9.00	---	2	---	1	1	---	NARROW BRIDGE WORD MESSAGE
	3-07	MR5-54R	REMOVE			---	---	---	---	---	1	1	---	TO RUSTIC ROAD [1] MILE/RIGHT ARROW
	3-08	W14-3	48	X	36	---	6.00	---	1	---	1	1	3-09	NO PASSING ZONE
	3-09	J1-1	24	X	39	6.50	---	---	---	---	1	---	3-08	JUNCTION OR END ASSEMBLY
	3-10	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	3-11	W2-1	30	X	30	---	6.25	---	1	---	1	1	---	CROSS ROAD
	3-12	I55-56	30	X	36	7.50	---	---	1	---	1	2	---	ADOPT A HIGHWAY SPONSOR
	3-13	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	3-14	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	3-15	J4-1	24	X	36	6.00	---	---	1	---	---	---	---	REASSURANCE ASSEMBLY
	3-16	J4-1	24	X	36	6.00	---	---	1	---	---	---	---	REASSURANCE ASSEMBLY
	4-01	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	4-02	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	4-03	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	4-04	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	4-05	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	4-06	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	4-07	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	4-08	W5-52R	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN LEFT
	4-09	W5-52L	12	X	36	---	3.00	1	---	---	1	1	---	CLEARANCE STRIPER DOWN RIGHT
	4-10	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	5-01	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	5-02	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	5-03	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	6-01	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	6-02	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	SUBTOTAL					177.96	135.50	20	32	3	50	51		

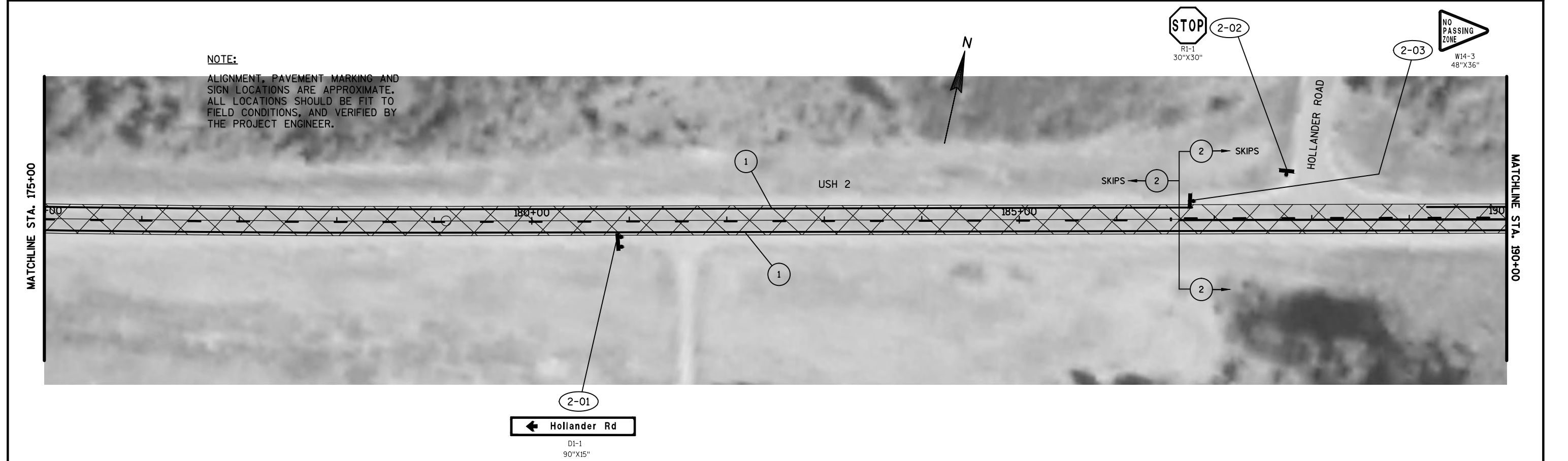
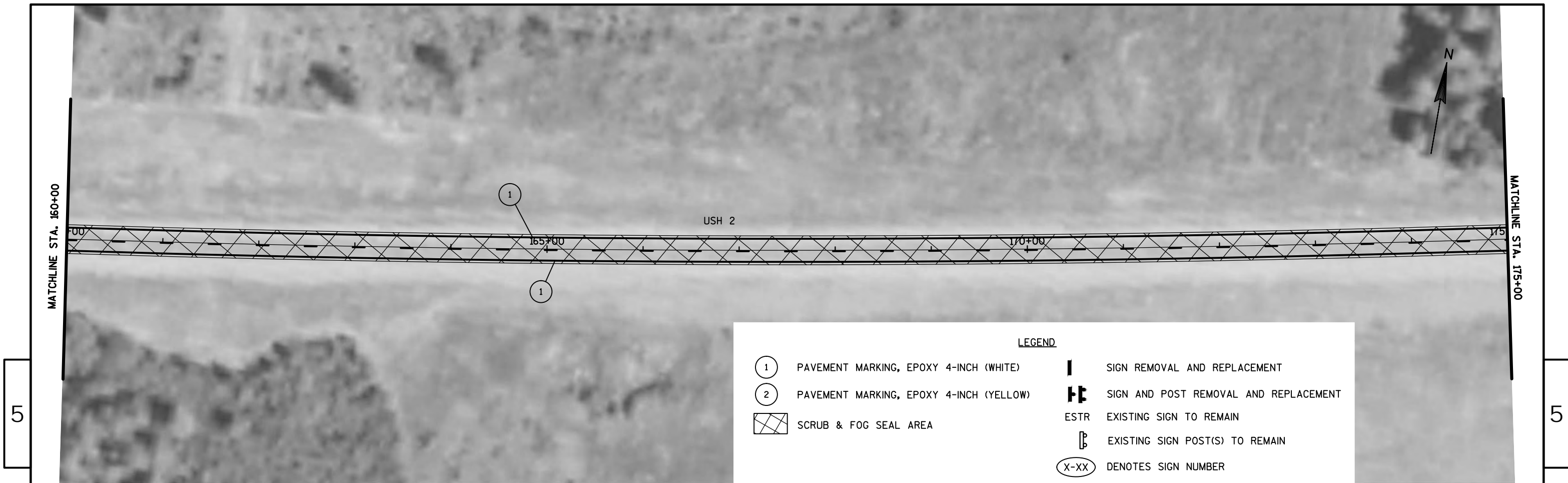
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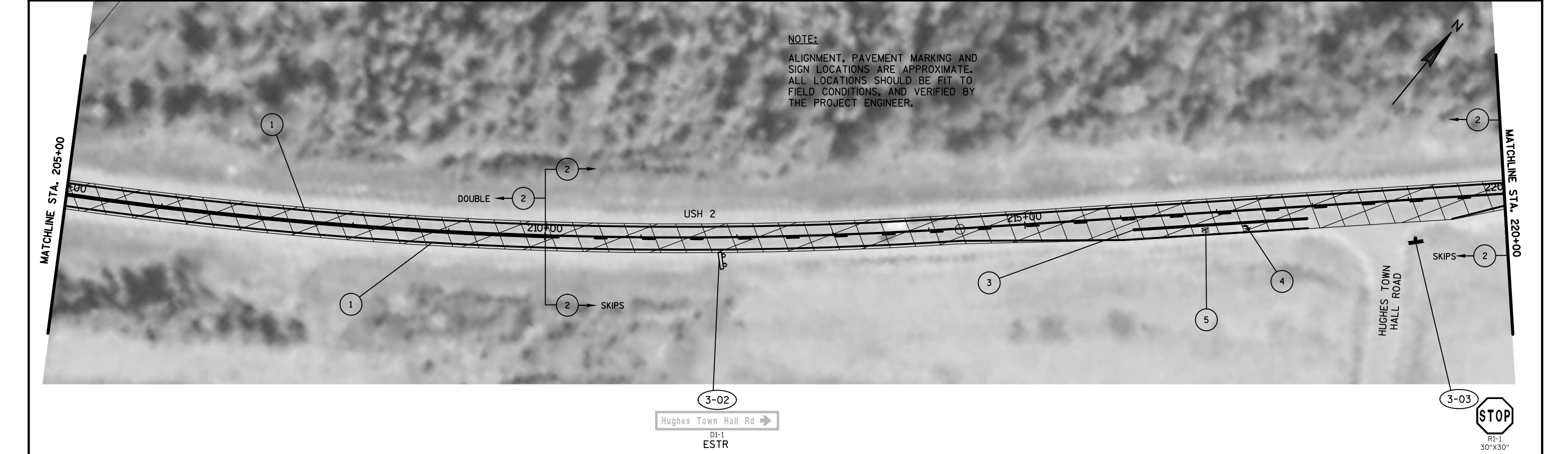
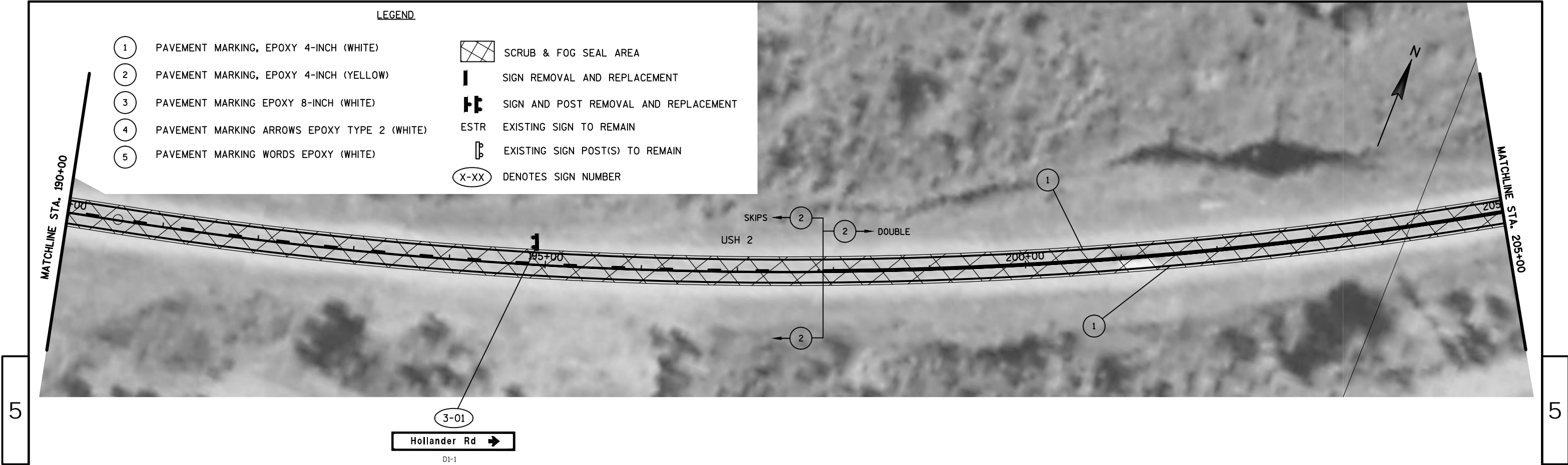
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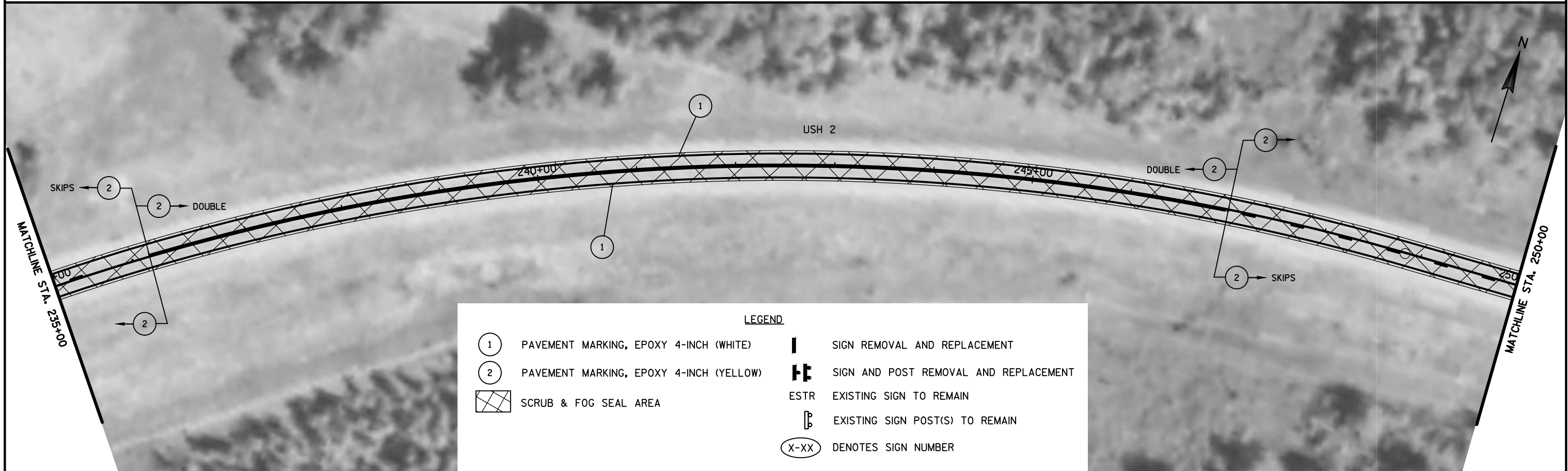
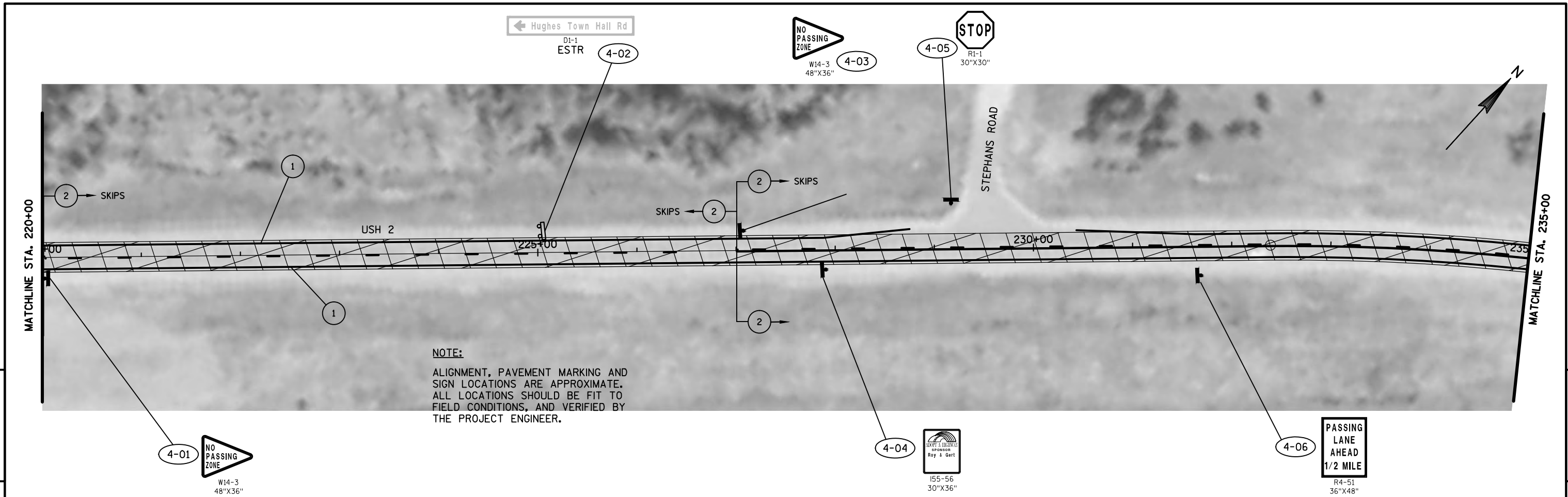
STH 48 PERMANENT SIGNING TYPE II (CONTINUED)														
CATEGORY	SIGN #	SIGN CODE	W IN	SIGN SIZE		637.2210	637.2230	634.0614	634.0616	634.0618	638.2602	638.3000	REMARKS	
						SIGNS	SIGNS	POSTS WOOD 4X6-INCH			REMOVING	REMOVING		
						TYPE II	TYPE II				SIGNS	SMALL SIGN		
						REFLECTIVE	REFLECTIVE	14 FT	16 FT	18 FT	TYPE II	SUPPORTS	SIGN MOUNTED	
						H SF	F SF	EACH	EACH	EACH	EACH	EACH	ON SAME POST AS	
0010	SUBTOTAL					177.96	135.50	20	32	3	50	51		
	6-03	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	6-04	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	7-01	W2-1	30	X	30	---	6.25	---	1	---	1	1	---	CROSS ROAD
	7-02	J4-1	24	X	36	6.00	---	---	1	---	1	1	---	REASSURANCE ASSEMBLY
	7-03	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	7-04	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	7-05	I55-56	30	X	36	7.50	---	---	1	---	1	1	--	ADOPT A HIGHWAY SPONSOR
	7-06	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	7-07	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	7-08	I55-56	30	X	36	7.50	---	---	1	---	1	1	---	ADOPT A HIGHWAY SPONSOR
	8-01	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	8-02	W14-3	48	X	36	---	6.00	---	1	---	1	1	---	NO PASSING ZONE
	8-03	R1-1	30	X	30	5.18	---	1	---	---	1	1	---	STOP SIGN
	8-04	W3-5	36	X	36	---	9.00	---	2	---	1	1	---	SPEED LIMIT 45 AHEAD (ARROW)
	8-05	W12-2	36	X	36	---	9.00	---	2	---	1	1	---	LOW CLEARANCE SYMBOL
	8-06	R2-1	24	X	30	5.00	---	1	---	---	1	1	---	SPEED LIMIT 45 MPH
	8-07	R2-1	24	X	30	5.00	---	1	---	---	1	1	---	SPEED LIMIT 55 MPH
	8-08	R4-7		ESTR		---	---	---	---	---	---	---	---	KEEP RIGHT
	8-09	R5-1A		ESTR		---	---	---	---	---	---	---	---	WRONG WAY
	8-10	W6-3		ESTR		---	---	---	---	---	---	---	---	TWO WAY TRAFFIC
	8-11	J1-1		ESTR		---	---	---	---	---	---	---	---	JUNCTION OR END ASSEMBLY
	8-12	R5-1A		ESTR		---	---	---	---	---	---	---	---	WRONG WAY
	8-13	R5-1A		ESTR		---	---	---	---	---	---	---	---	WRONG WAY
	8-14	J2-3		ESTR		---	---	---	---	---	---	---	---	ROUTE TURN ASSEMBLY
PROJECT 8120-00-60 TOTAL						234.86	183.75	27	44	3	67	68		

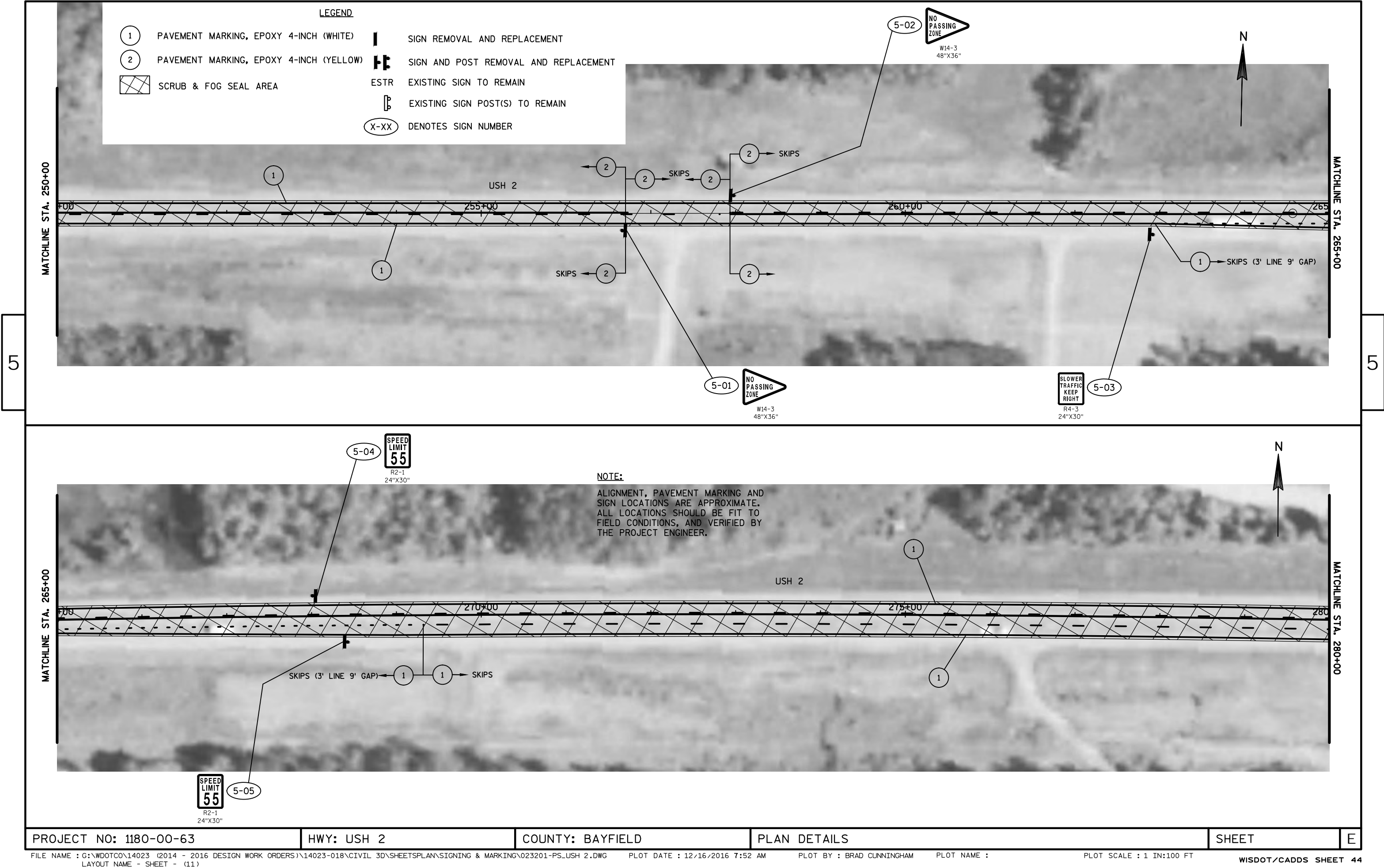


PROJECT NO: 1180-00-63	HWY: USH 2	COUNTY: BAYFIELD	PLAN DETAILS	SHEET	E
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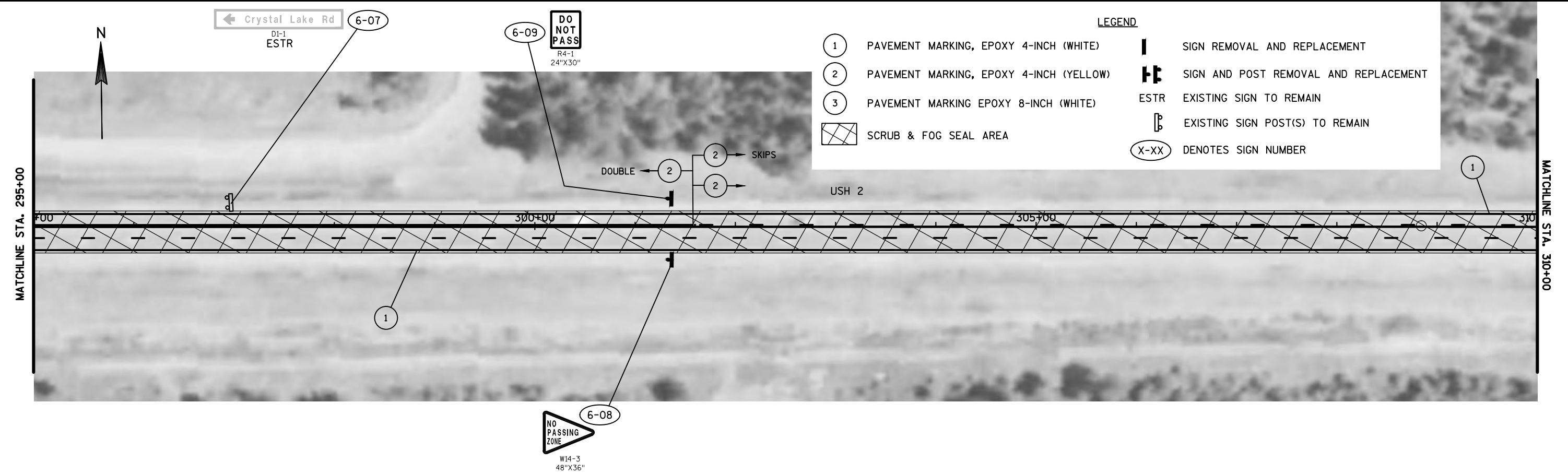
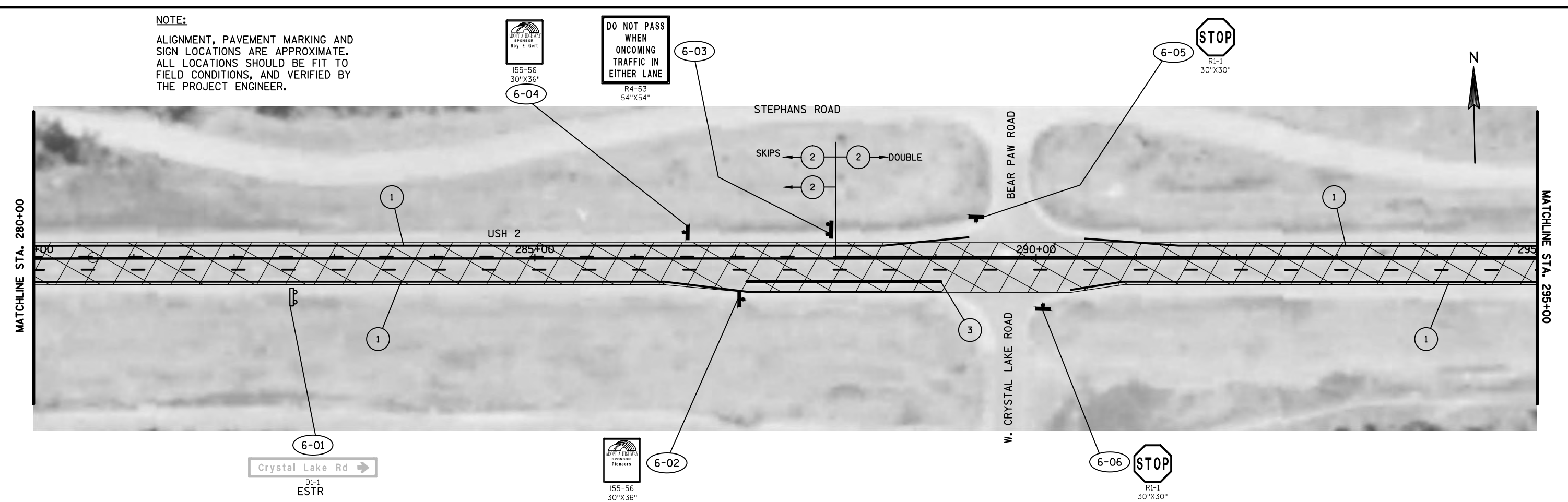


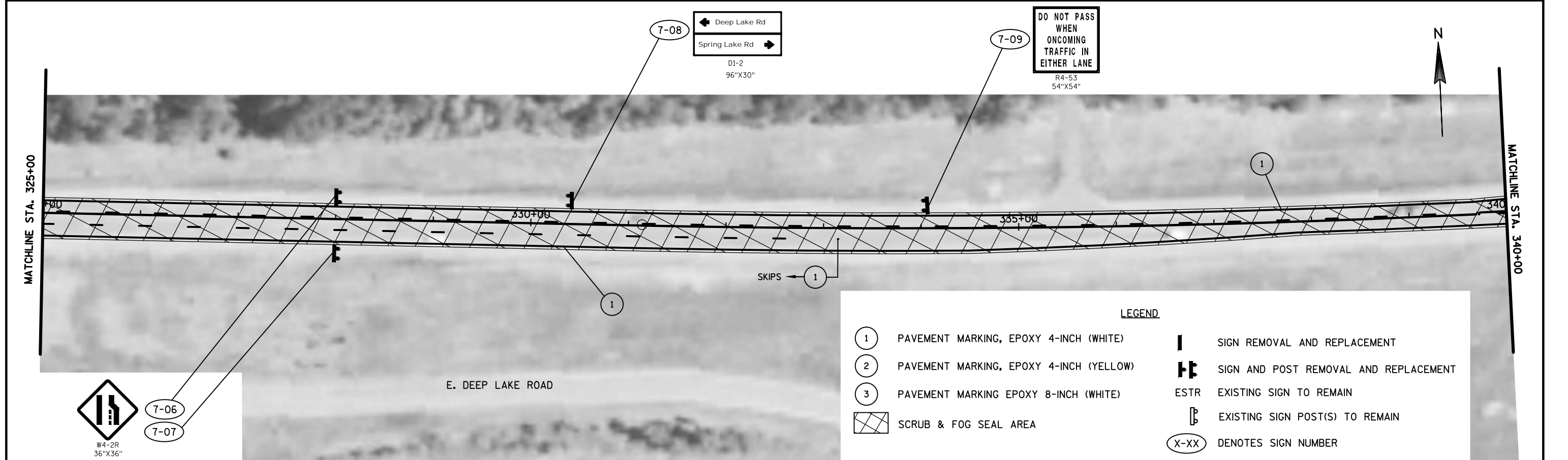
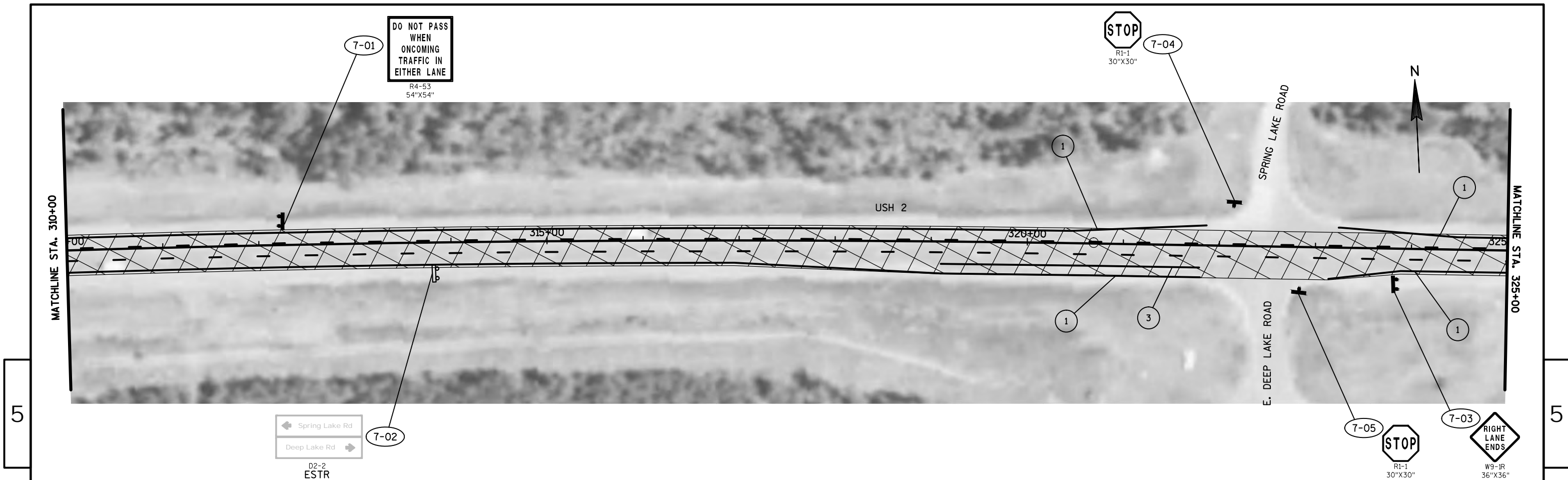




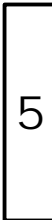










NOTE:
ALIGNMENT, PAVEMENT MARKING AND
SIGN LOCATIONS ARE APPROXIMATE.
ALL LOCATIONS SHOULD BE FIT TO
FIELD CONDITIONS, AND VERIFIED BY
THE PROJECT ENGINEER.

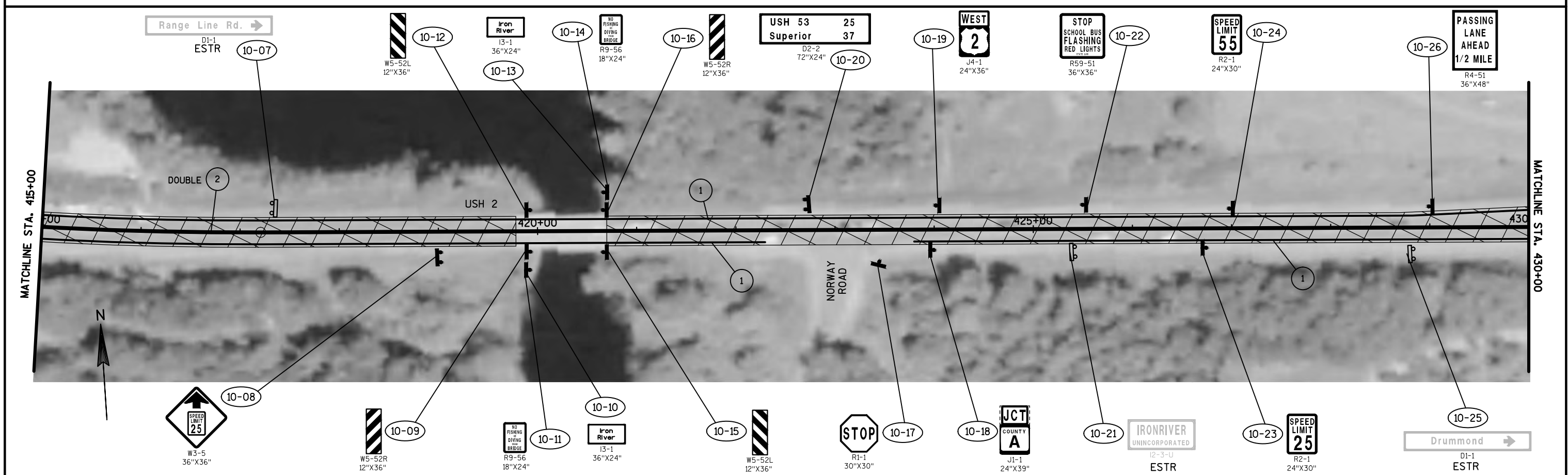
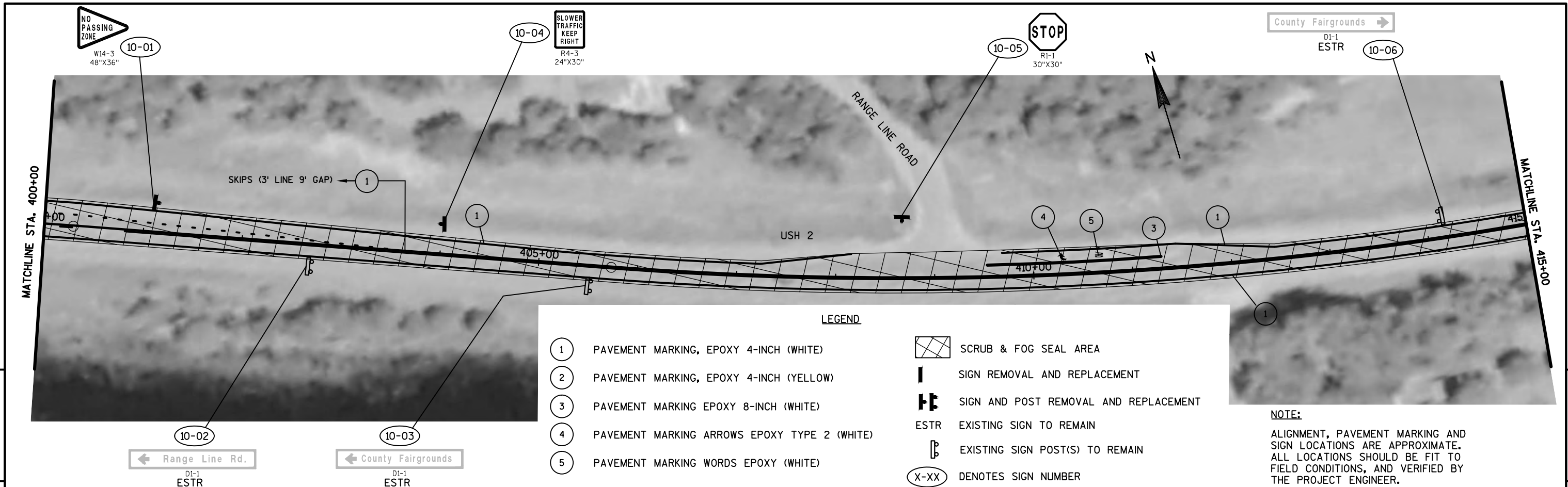


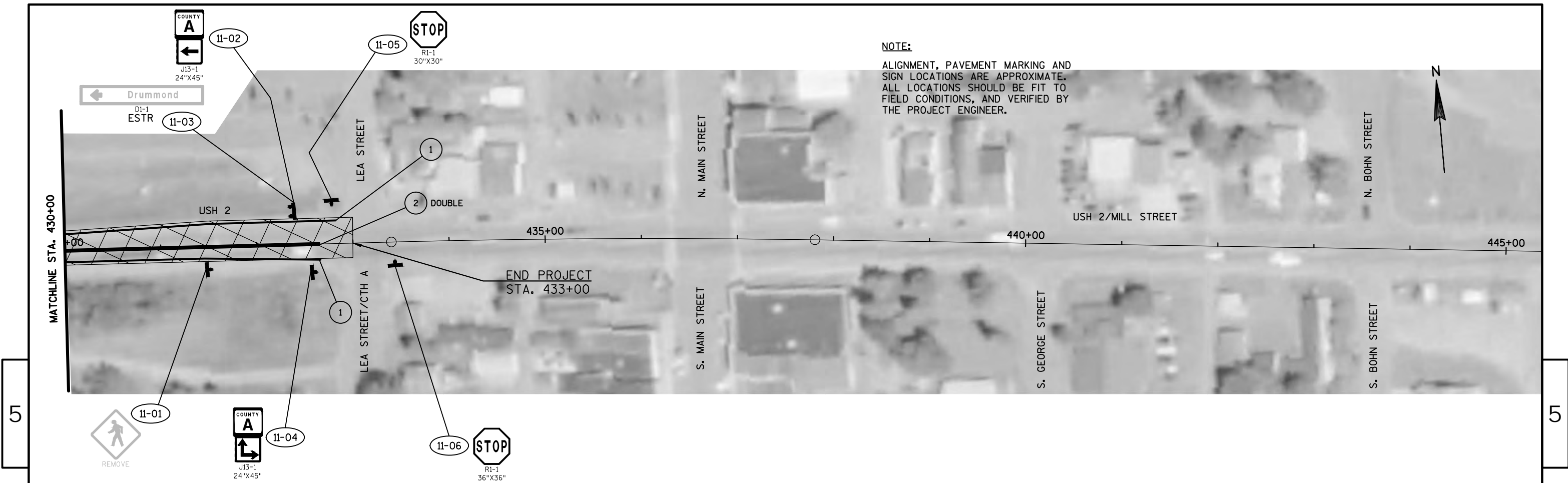


ALIGNMENT, PAVEMENT MARKING AND
SIGN LOCATIONS ARE APPROXIMATE.
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FIELD CONDITIONS, AND VERIFIED BY
THE PROJECT ENGINEER.



	PAVEMENT MARKING, EPOXY 4-INCH (WHITE)		SIGN REMOVAL AND REPLACEMENT
	PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)		SIGN AND POST REMOVAL AND REPLACEMENT
	PAVEMENT MARKING EPOXY 8-INCH (WHITE)	ESTR	EXISTING SIGN TO REMAIN
	SCRUB & FOG SEAL AREA		EXISTING SIGN POST(S) TO REMAIN
			DENOTES SIGN NUMBER



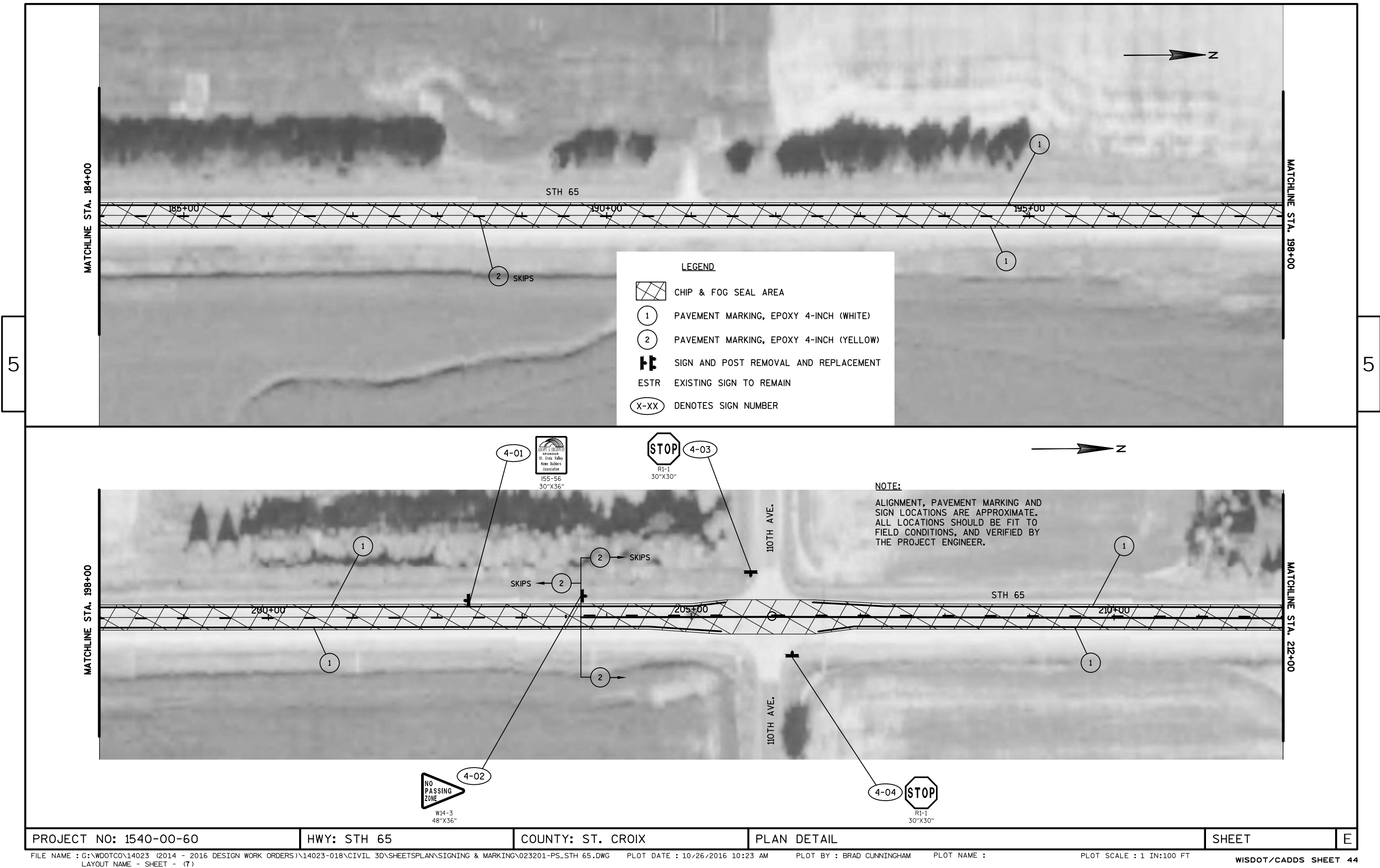


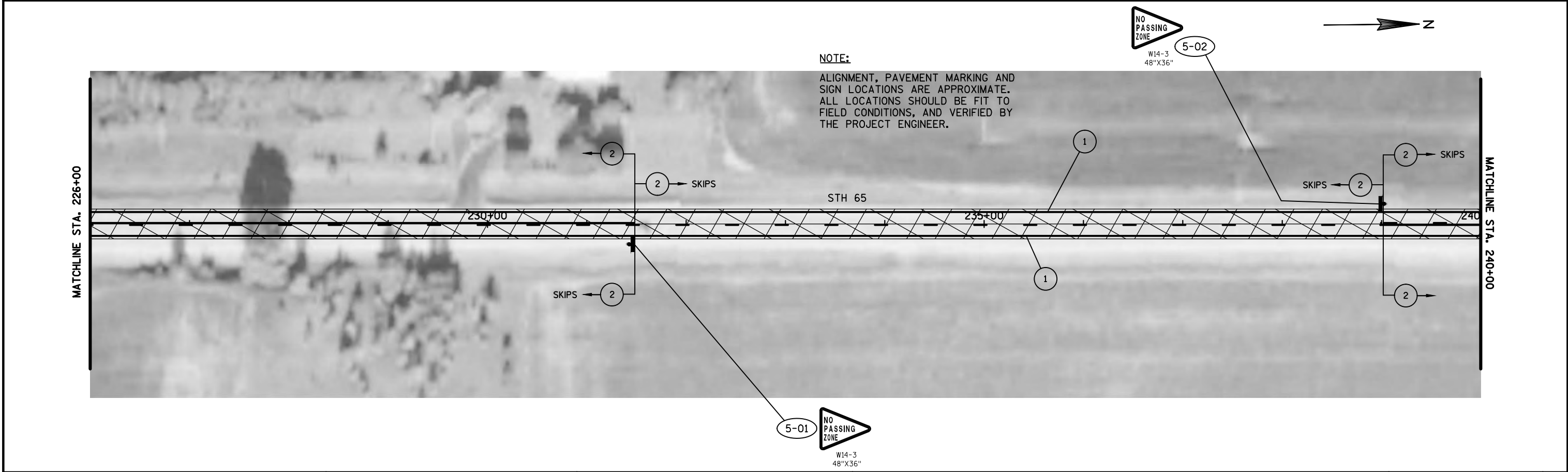
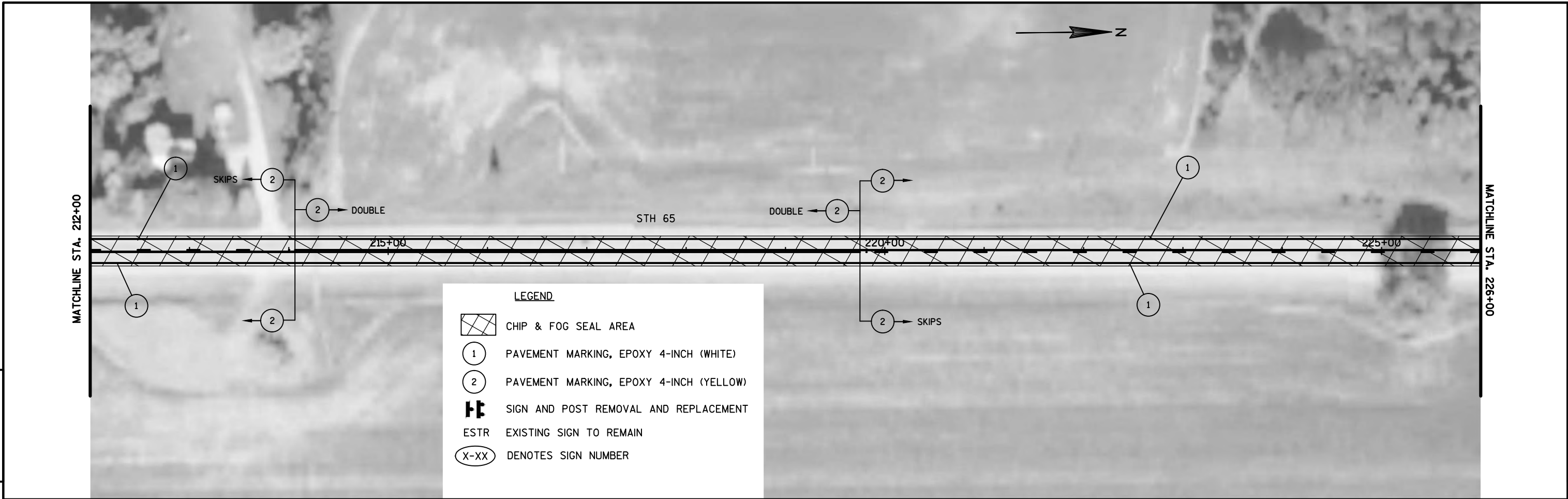
NOTE:
ALIGNMENT, PAVEMENT MARKING AND
SIGN LOCATIONS ARE APPROXIMATE.
ALL LOCATIONS SHOULD BE FIT TO
FIELD CONDITIONS, AND VERIFIED BY
THE PROJECT ENGINEER.

LEGEND

- | | | | |
|---|---|------|---------------------------------------|
| ① | PAVEMENT MARKING, EPOXY 4-INCH (WHITE) | | SCRUB & FOG SEAL AREA |
| ② | PAVEMENT MARKING, EPOXY 4-INCH (YELLOW) | | SIGN REMOVAL AND REPLACEMENT |
| | | | SIGN AND POST REMOVAL AND REPLACEMENT |
| | | ESTR | EXISTING SIGN TO REMAIN |
| | | | EXISTING SIGN POST(S) TO REMAIN |
| | | X-XX | DENOTES SIGN NUMBER |

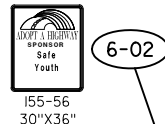
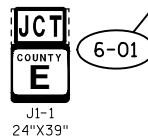






NOTE:

ALIGNMENT, PAVEMENT MARKING AND
SIGN LOCATIONS ARE APPROXIMATE.
ALL LOCATIONS SHOULD BE FIT TO
FIELD CONDITIONS, AND VERIFIED BY
THE PROJECT ENGINEER.

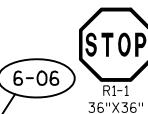


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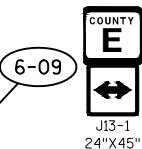
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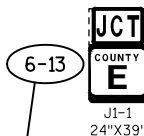
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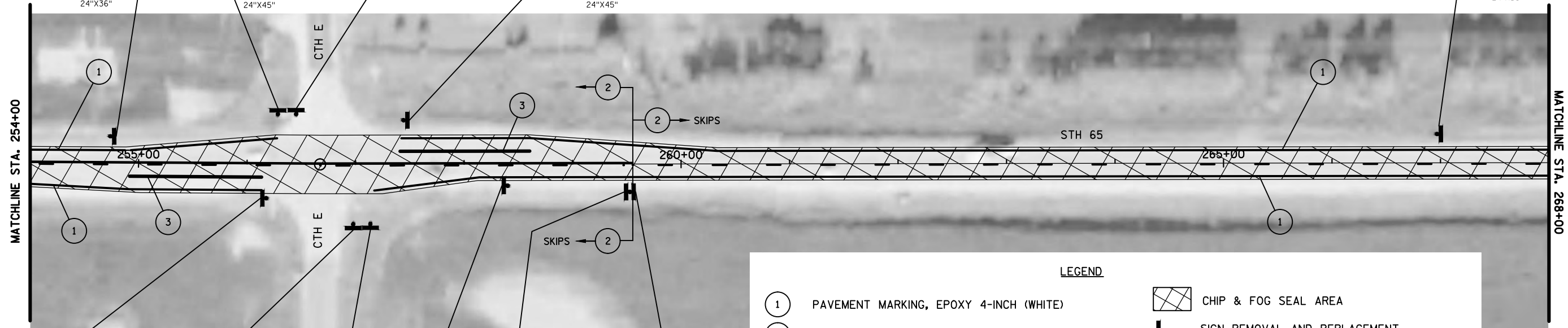
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6-09



6-13



6-04



6-07



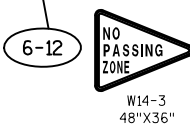
6-08



6-10



6-11



6-12

LEGEND

- 1 PAVEMENT MARKING, EPOXY 4-INCH (WHITE)
- 2 PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)
- 3 PAVEMENT MARKING EPOXY 8-INCH (WHITE)

- CHIP & FOG SEAL AREA
- SIGN REMOVAL AND REPLACEMENT
- SIGN AND POST REMOVAL AND REPLACEMENT
- EXISTING SIGN TO REMAIN
- XX DENOTES SIGN NUMBER

PROJECT NO: 1540-00-60

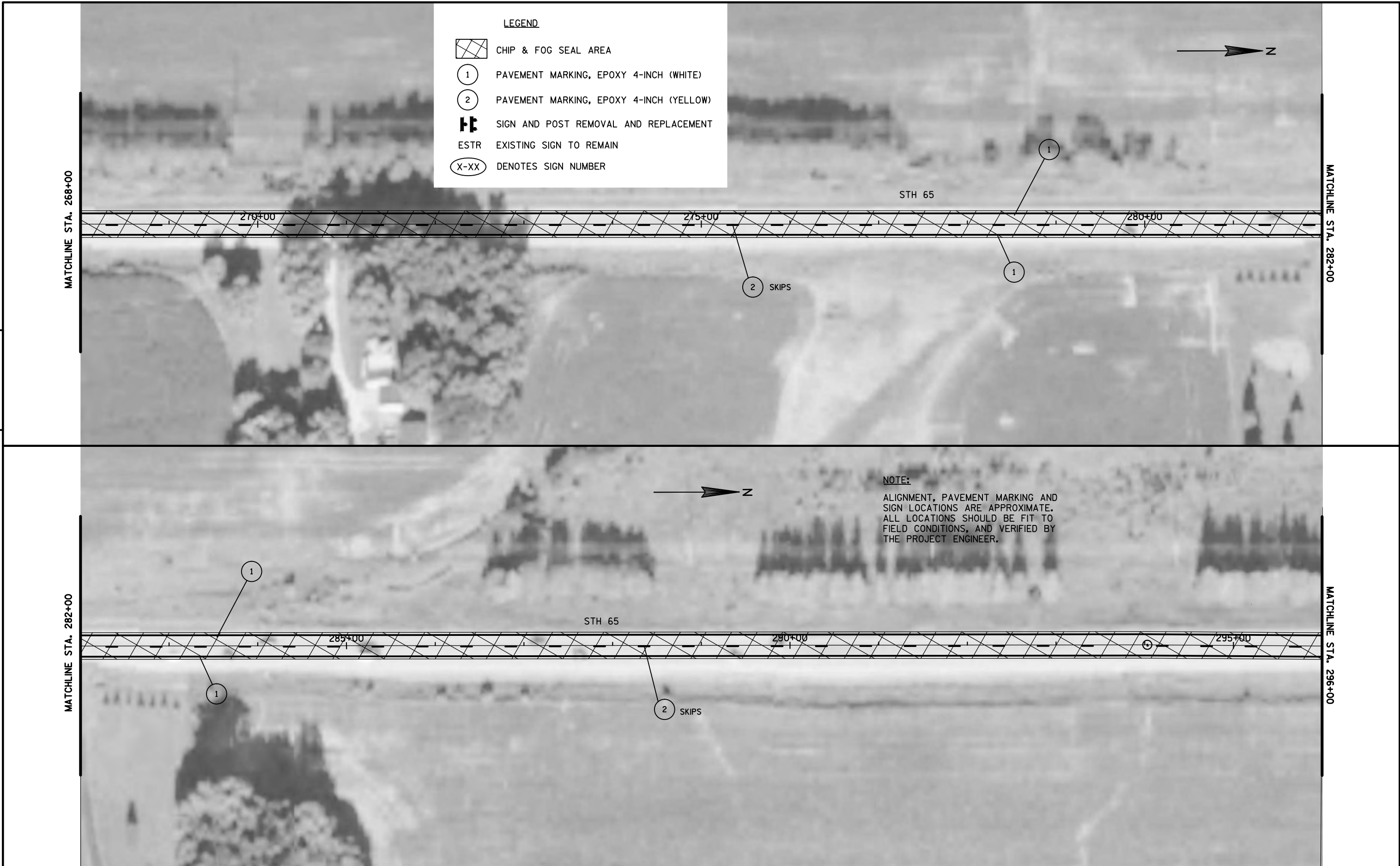
HWY: STH 65

COUNTY: ST. CROIX

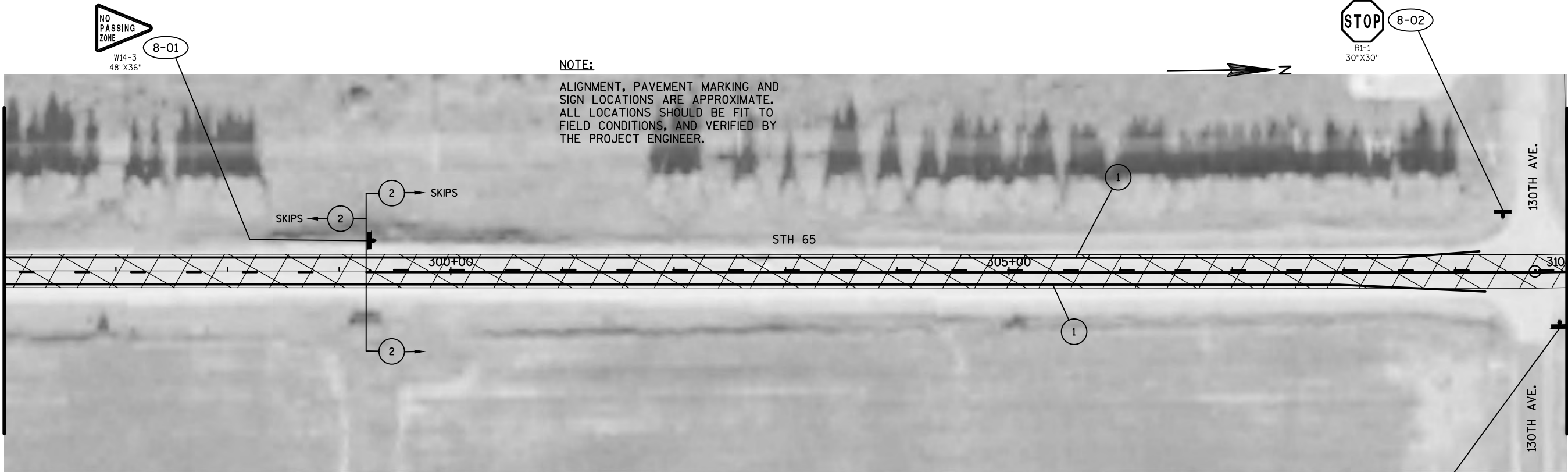
PLAN DETAIL

SHEET

E



5



5



PROJECT NO: 1540-00-60

HWY: STH 65

COUNTY: ST. CROIX

PLAN DETAIL

SHEET

E

NOTE:
ALIGNMENT, PAVEMENT MARKING AND
SIGN LOCATIONS ARE APPROXIMATE.
ALL LOCATIONS SHOULD BE FIT TO
FIELD CONDITIONS, AND VERIFIED BY
THE PROJECT ENGINEER.








MATCHLINE STA. 324+00

MATCHLINE STA. 338+00

5

5

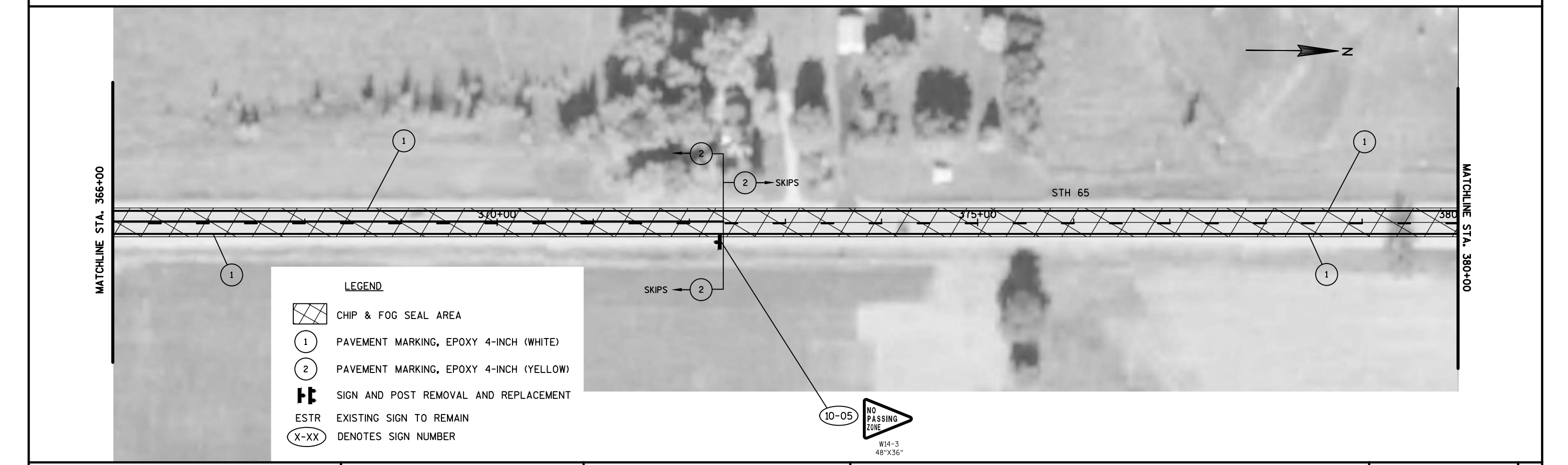
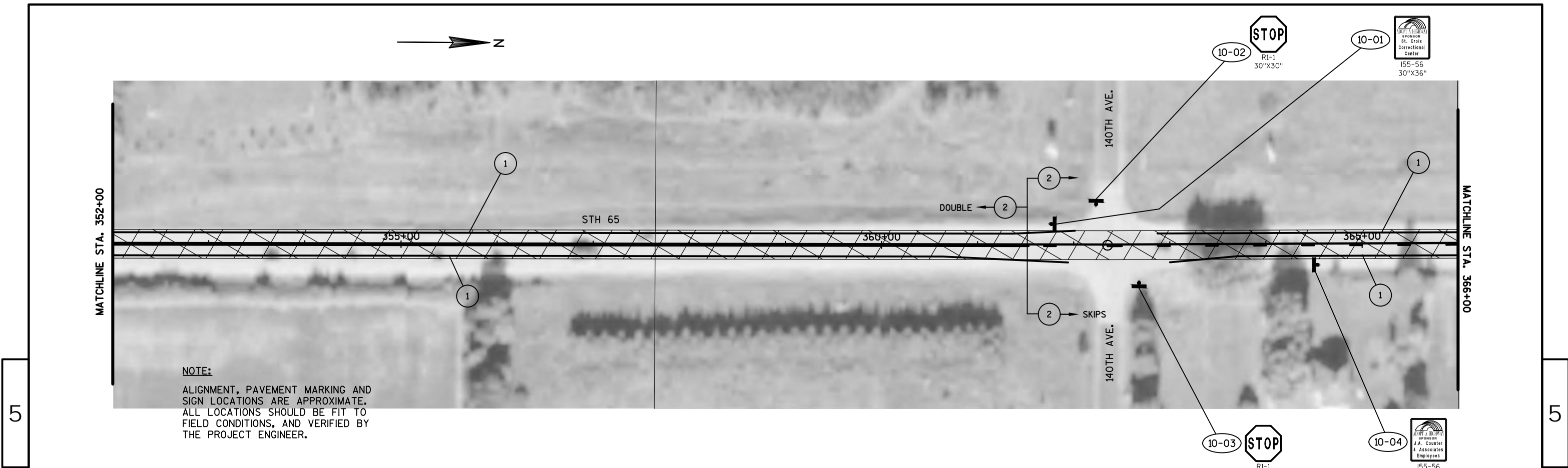
LEGEND

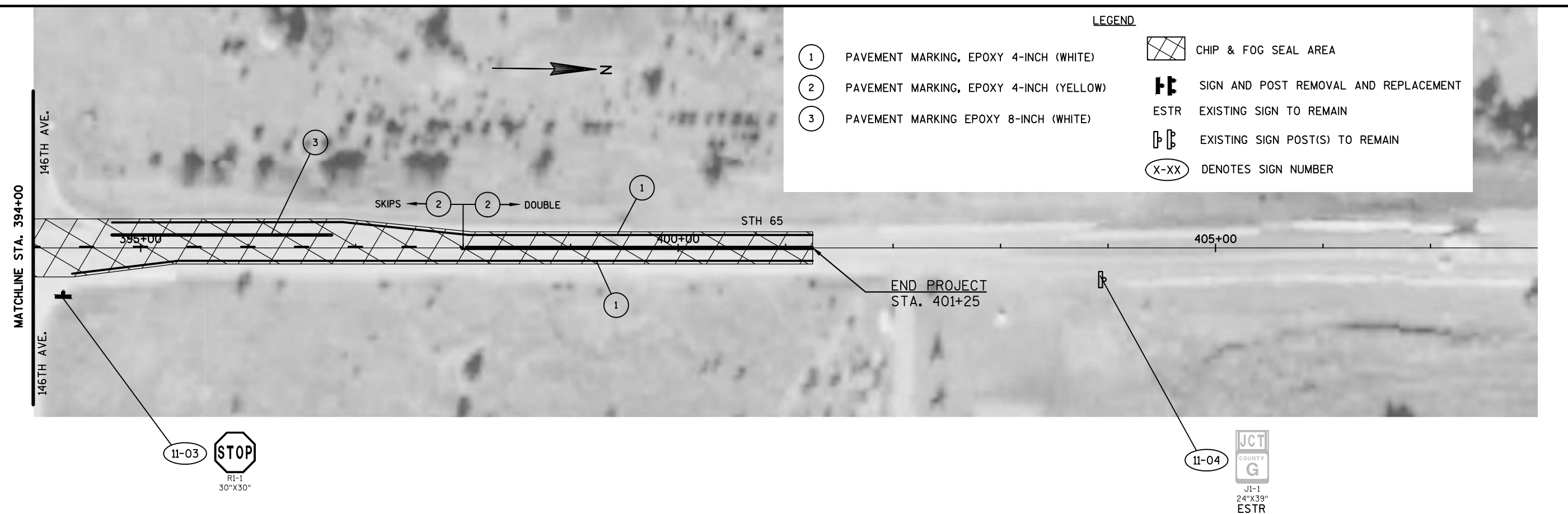
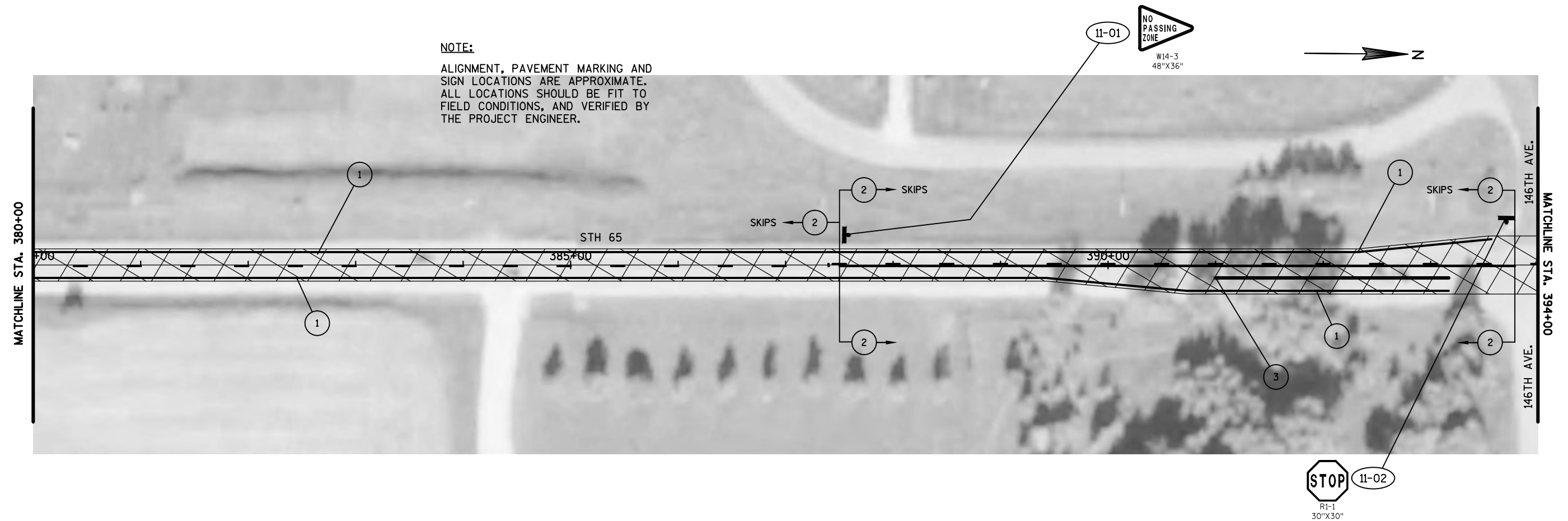
-  CHIP & FOG SEAL AREA
-  PAVEMENT MARKING, EPOXY 4-INCH (WHITE)
-  PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)
-  SIGN AND POST REMOVAL AND REPLACEMENT
- ESTR EXISTING SIGN TO REMAIN
-  DENOTES SIGN NUMBER

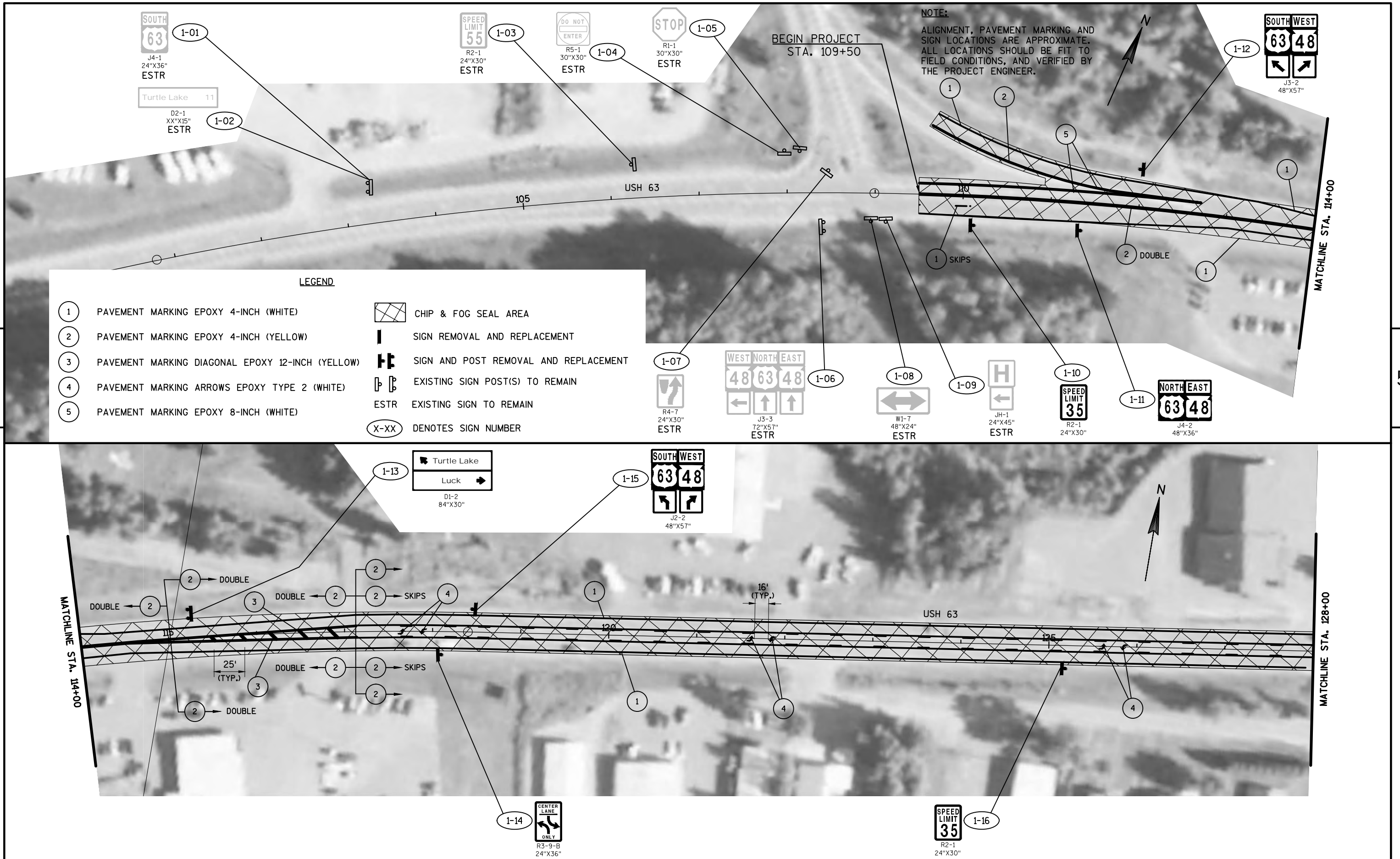
MATCHLINE STA. 338+00

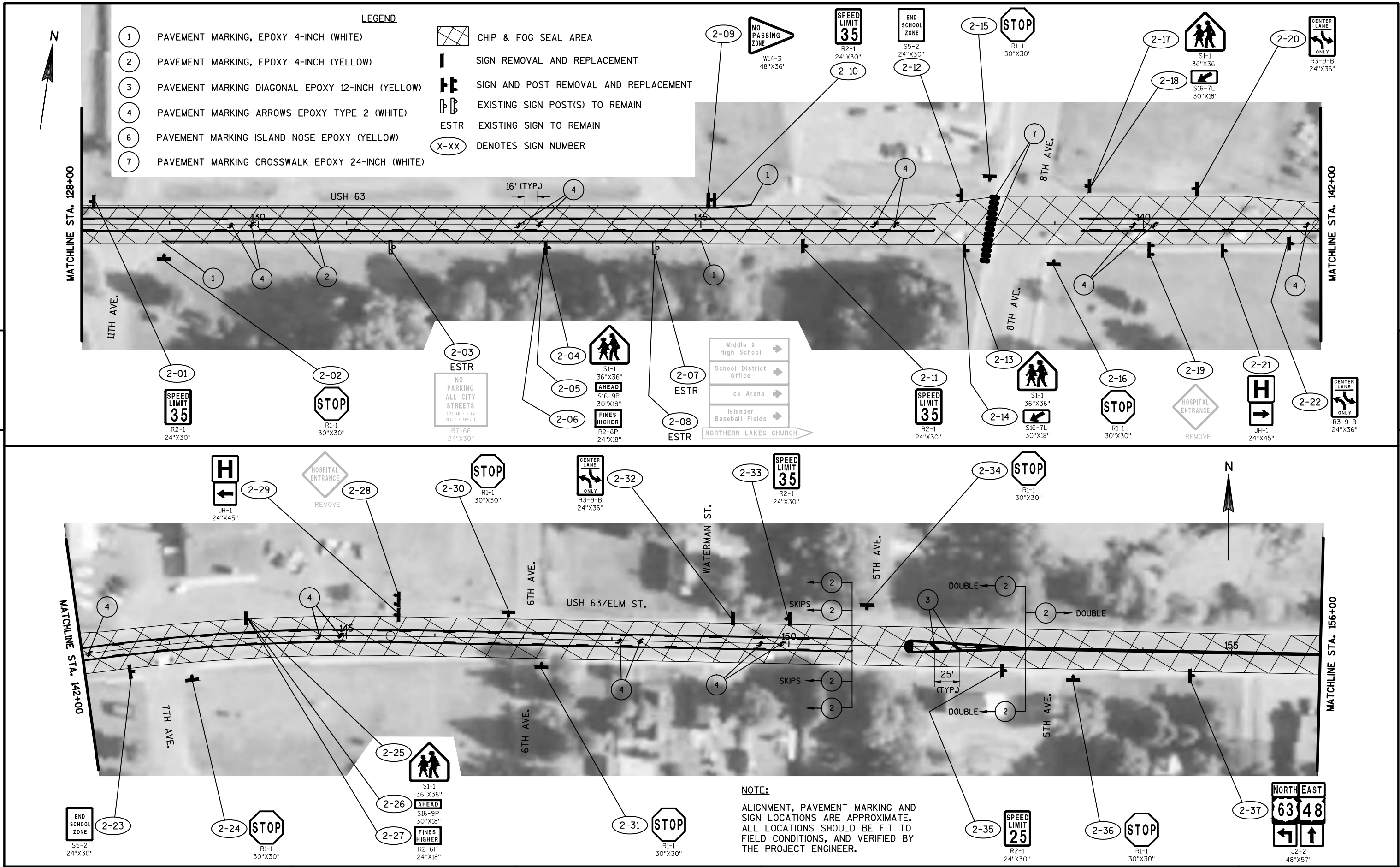
MATCHLINE STA. 352+00

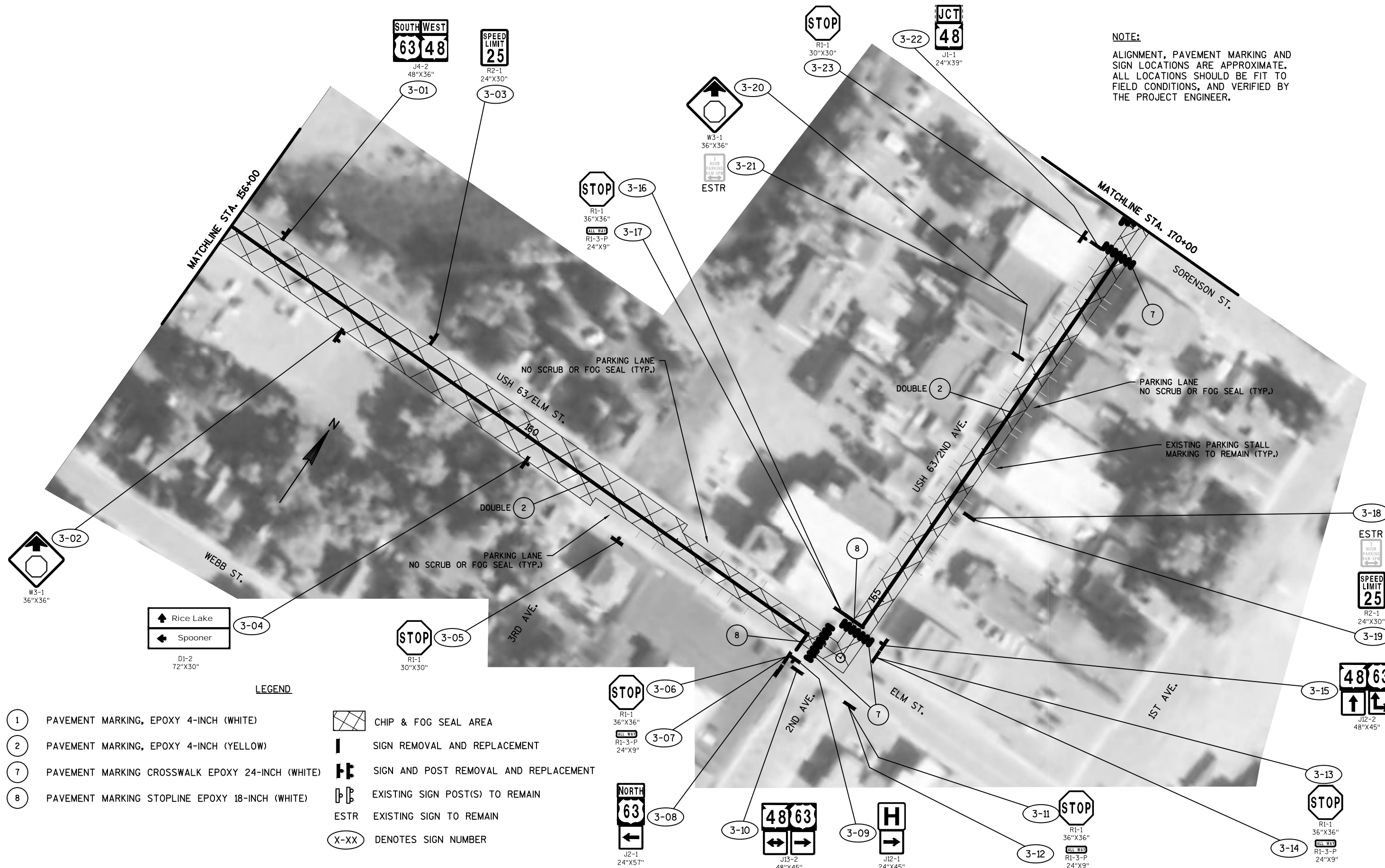


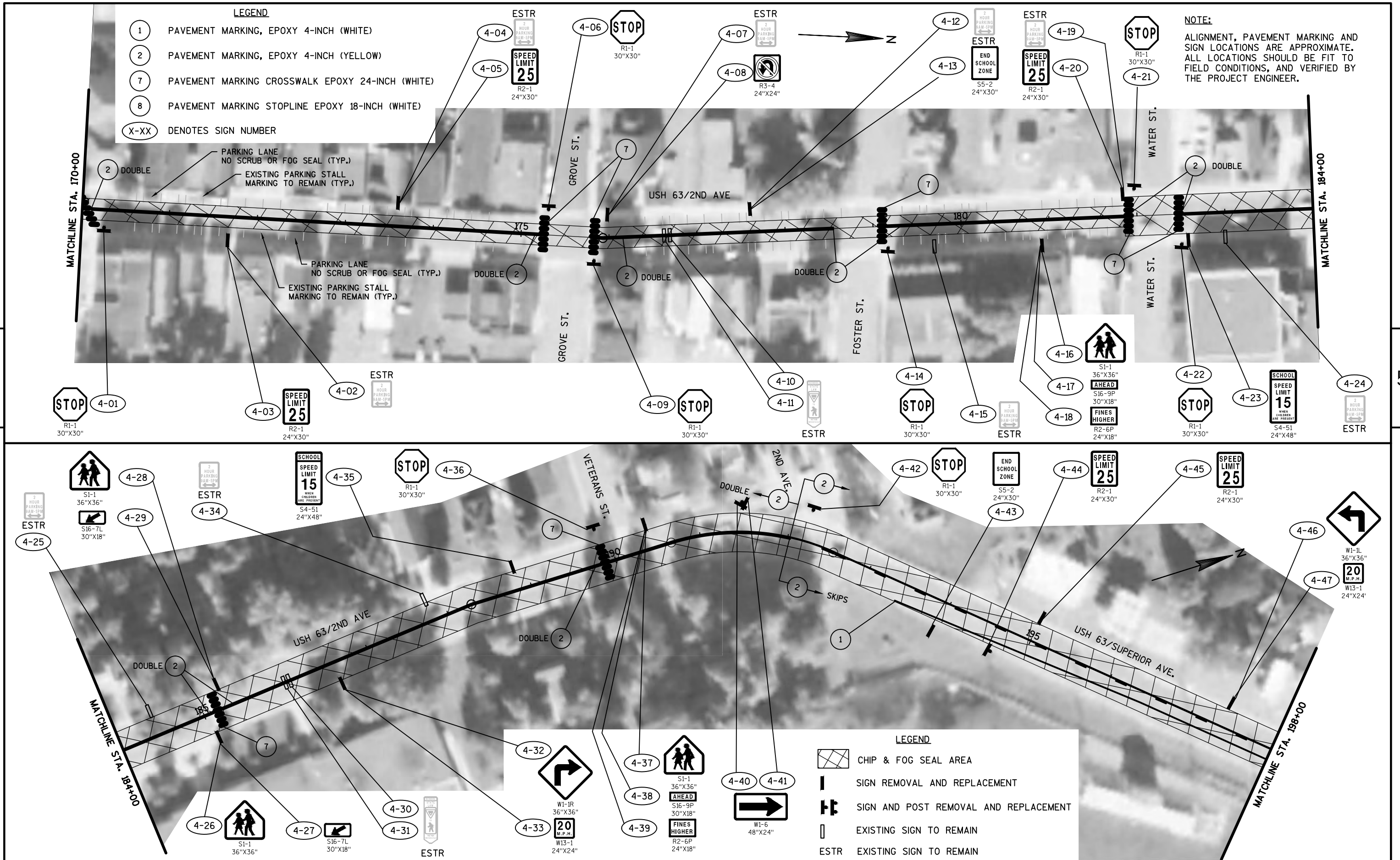


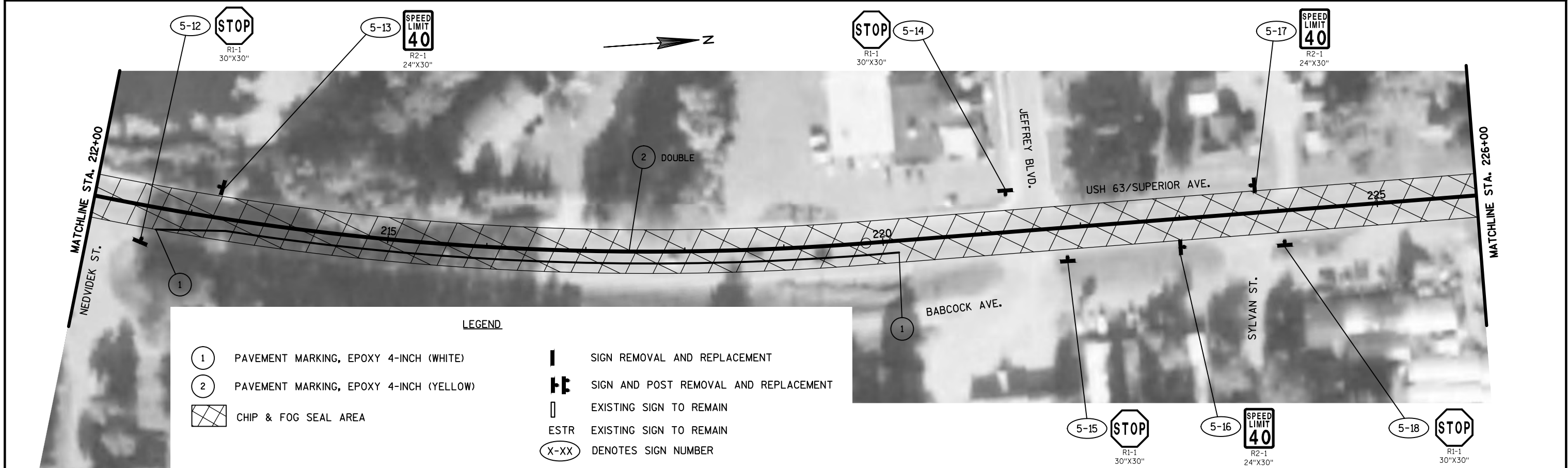
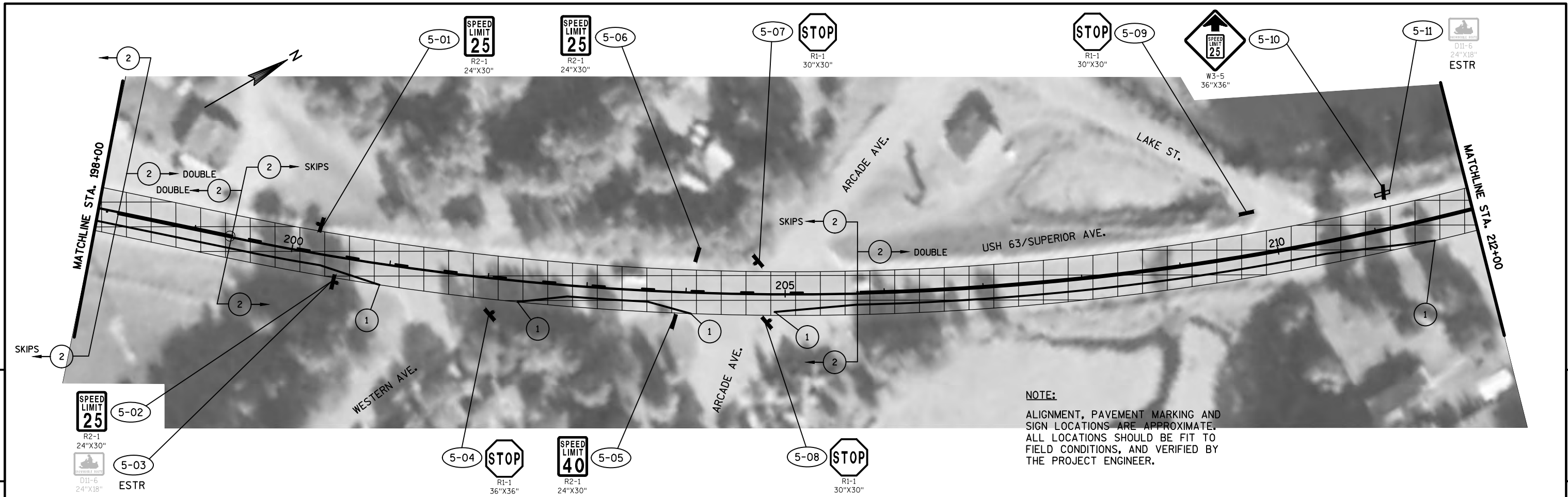




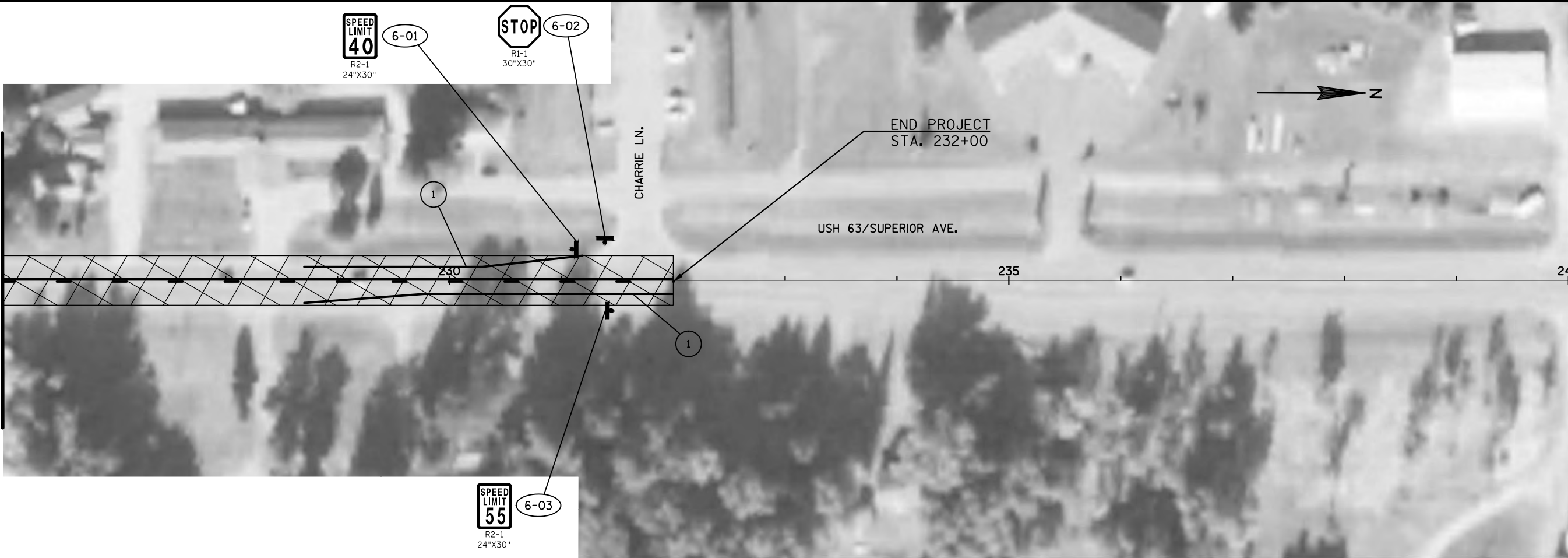








5



5

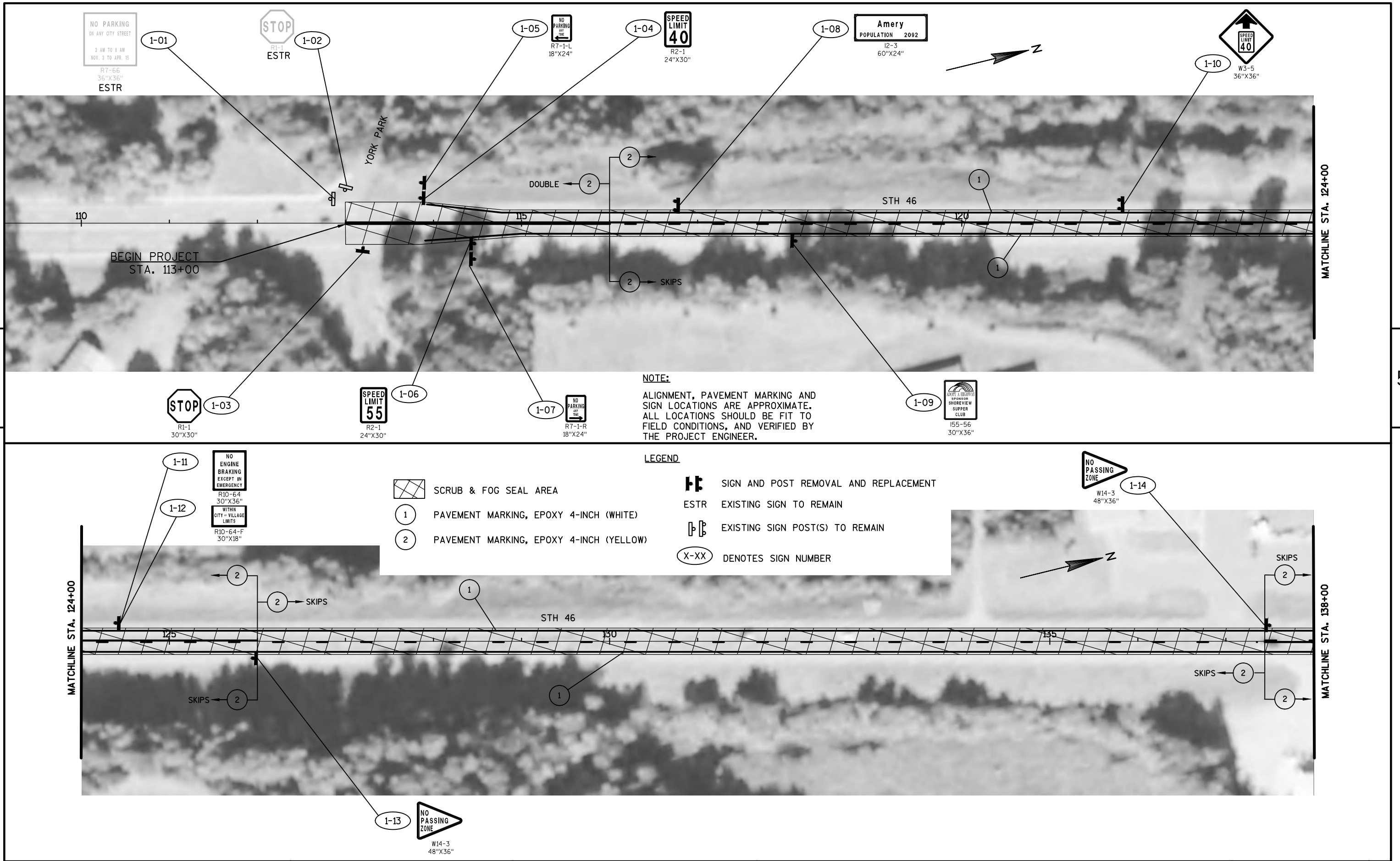
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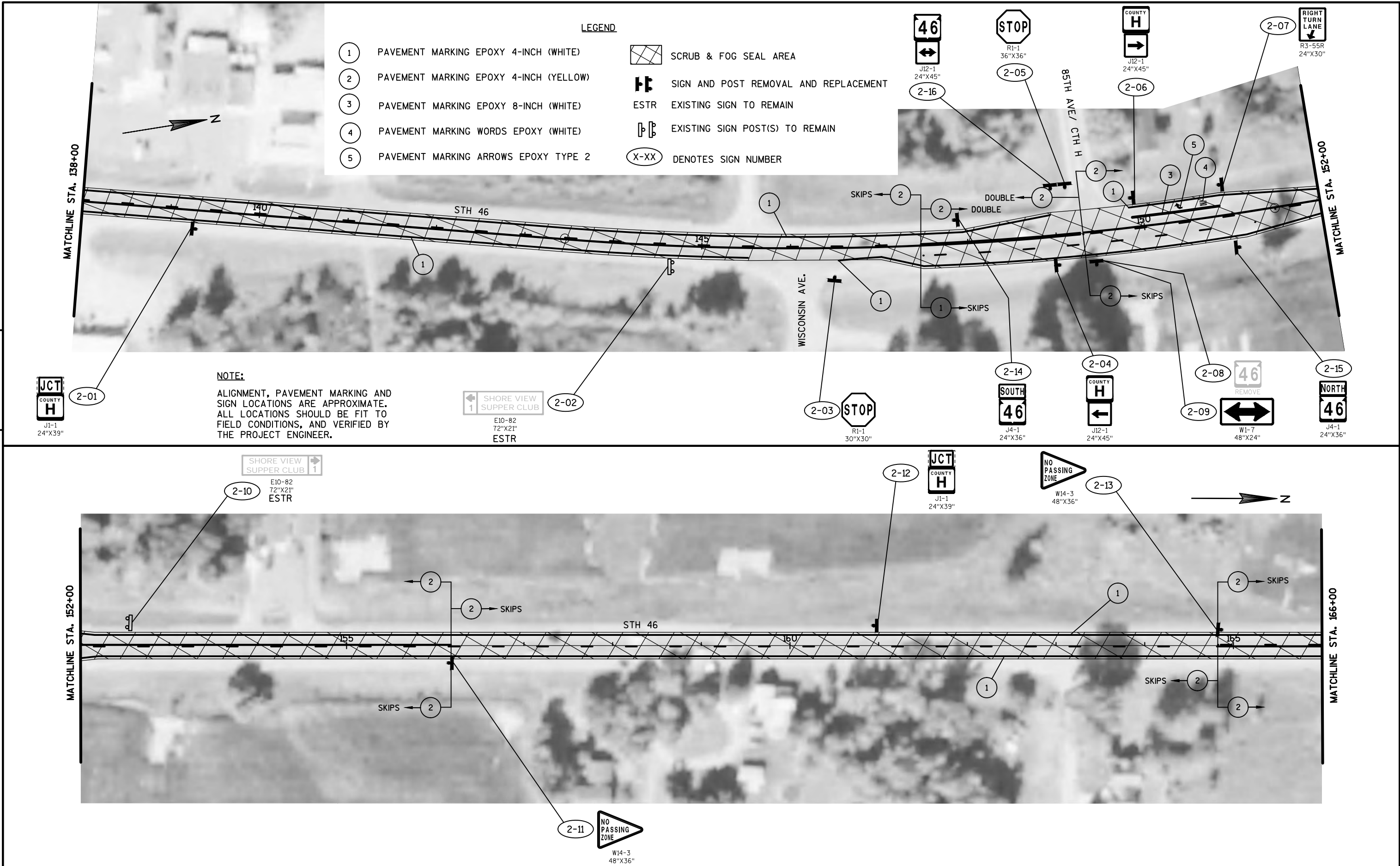
ALIGNMENT, PAVEMENT MARKING AND SIGN LOCATIONS ARE APPROXIMATE. ALL LOCATIONS SHOULD BE FIT TO FIELD CONDITIONS, AND VERIFIED BY THE PROJECT ENGINEER.

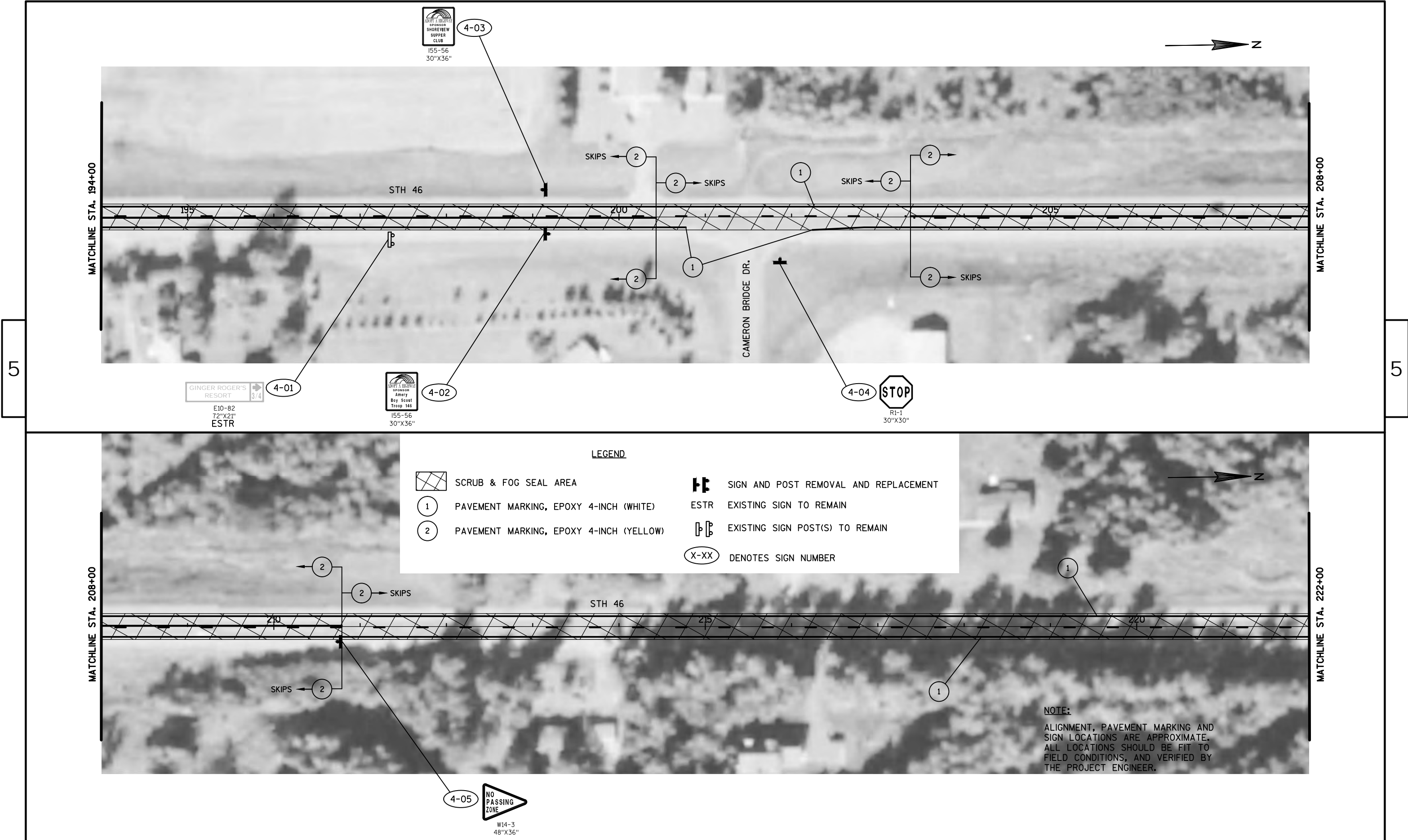
LEGEND

- ① PAVEMENT MARKING, EPOXY 4-INCH (WHITE)
- ② PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)

- CHIP & FOG SEAL AREA
- SIGN REMOVAL AND REPLACEMENT
- SIGN AND POST REMOVAL AND REPLACEMENT
- ESTR EXISTING SIGN TO REMAIN
- ⓧ-XX DENOTES SIGN NUMBER







PROJECT NO: 8070-00-60

HWY: STH 46

COUNTY: POLK

PLAN DETAILS

SHEET

E

NOTE:

ALIGNMENT, PAVEMENT MARKING AND
SIGN LOCATIONS ARE APPROXIMATE.
ALL LOCATIONS SHOULD BE FIT TO
FIELD CONDITIONS, AND VERIFIED BY
THE PROJECT ENGINEER.

MATCHLINE STA. 222+00

MATCHLINE STA. 236+00

2 SKIPS

STH 46

1

1

VIJOBI TRAIL

5-01



LEGEND



SCRUB & FOG SEAL AREA

1

PAVEMENT MARKING, EPOXY 4-INCH (WHITE)

2

PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)



SIGN AND POST REMOVAL AND REPLACEMENT

ESTR

EXISTING SIGN TO REMAIN



EXISTING SIGN POST(S) TO REMAIN

X-XX

DENOTES SIGN NUMBER

MATCHLINE STA. 236+00

MATCHLINE STA. 250+00

1

1

STH 46

SKIPS

2

2

2

SKIPS



5-02

W14-3
48\"/>

PROJECT NO: 8070-00-60

HWY: STH 46

COUNTY: POLK

PLAN DETAILS

SHEET

E

FILE NAME : G:\WDOTCO\14023 (2014 - 2016 DESIGN WORK ORDERS)\14023-018\CIVIL 3D\SHEETSPLAN\SIGNING & MARKING\023201-PS_STH 46.DWG
LAYOUT NAME - SHEET - (9)

PLOT DATE : 12/16/2016 10:20 AM

PLOT BY : BRAD CUNNINGHAM

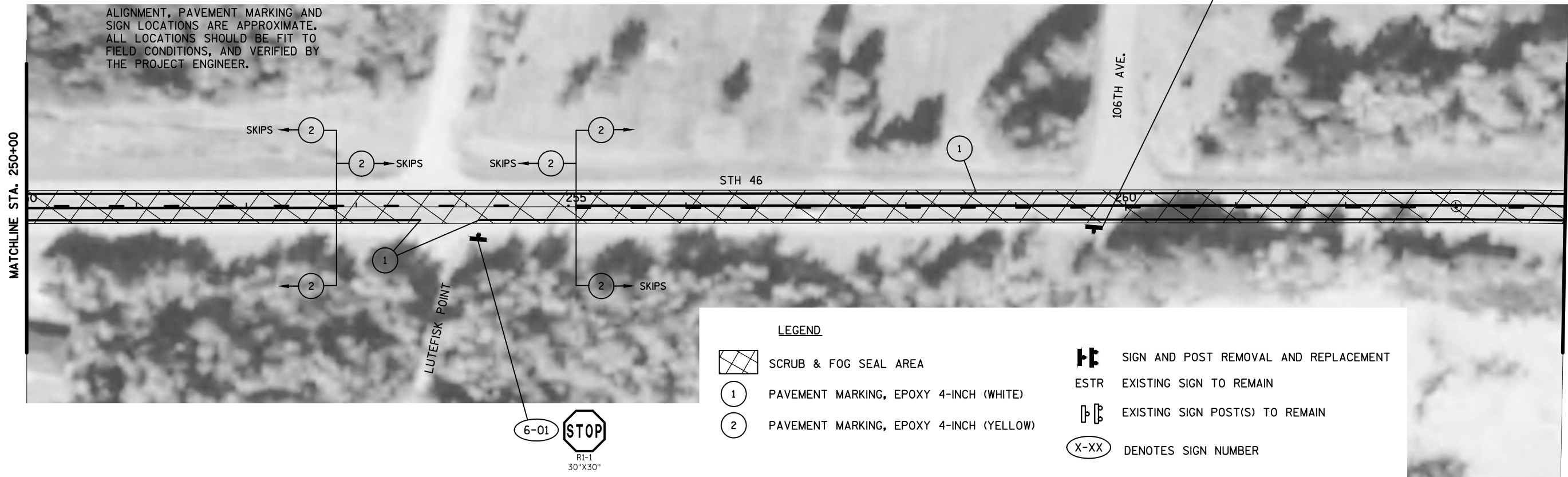
PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

NOTE:

ALIGNMENT, PAVEMENT MARKING AND SIGN LOCATIONS ARE APPROXIMATE. ALL LOCATIONS SHOULD BE FIT TO FIELD CONDITIONS, AND VERIFIED BY THE PROJECT ENGINEER.



LEGEND



SCRUB & FOG SEAL AREA

1

PAVEMENT MARKING, EPOXY 4-INCH (WHITE)

2

PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)



SIGN AND POST REMOVAL AND REPLACEMENT

ESTR

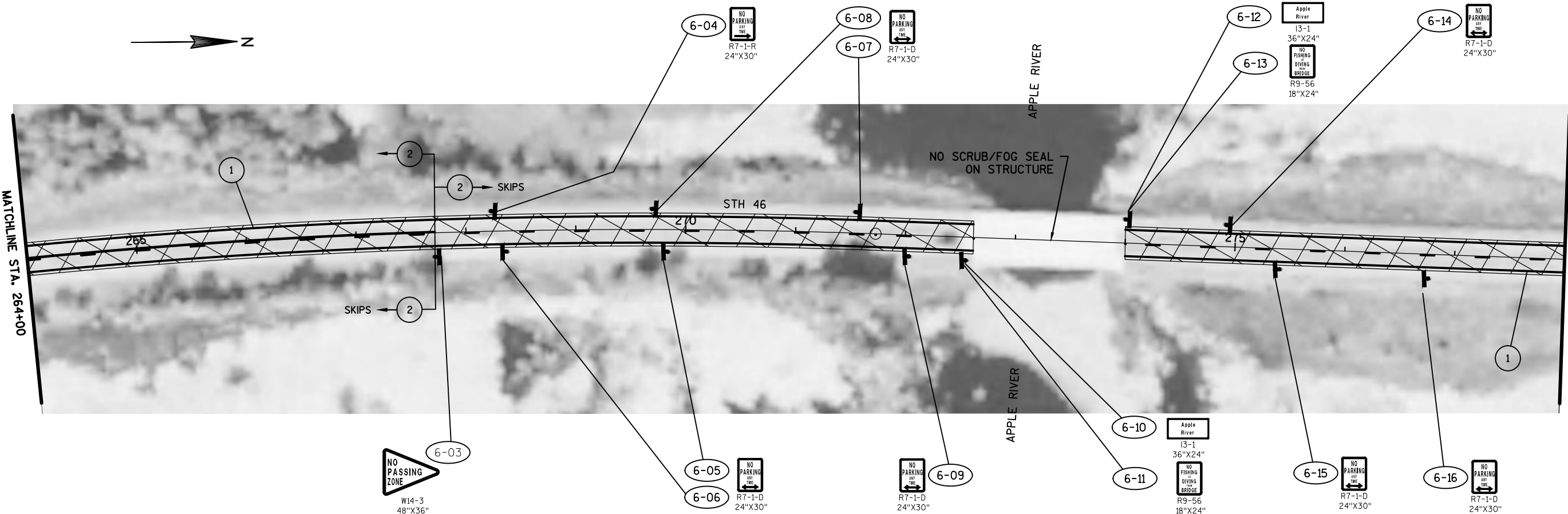
EXISTING SIGN TO REMAIN



EXISTING SIGN POST(S) TO REMAIN

X-XX

DENOTES SIGN NUMBER



PROJECT NO: 8070-00-60

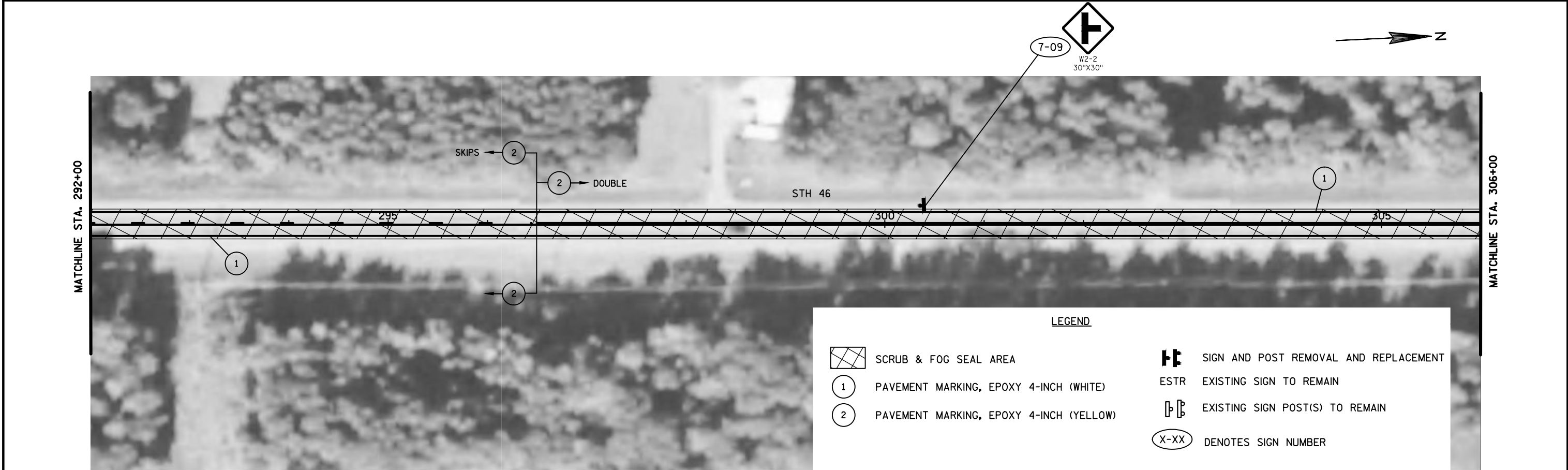
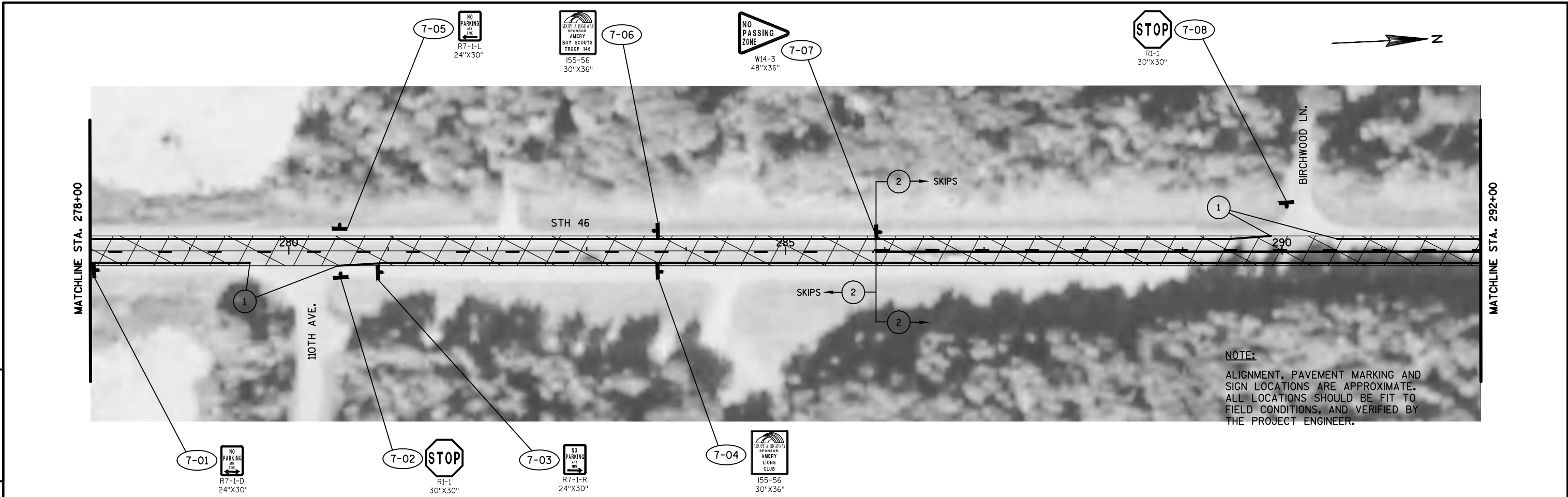
HWY: STH 46

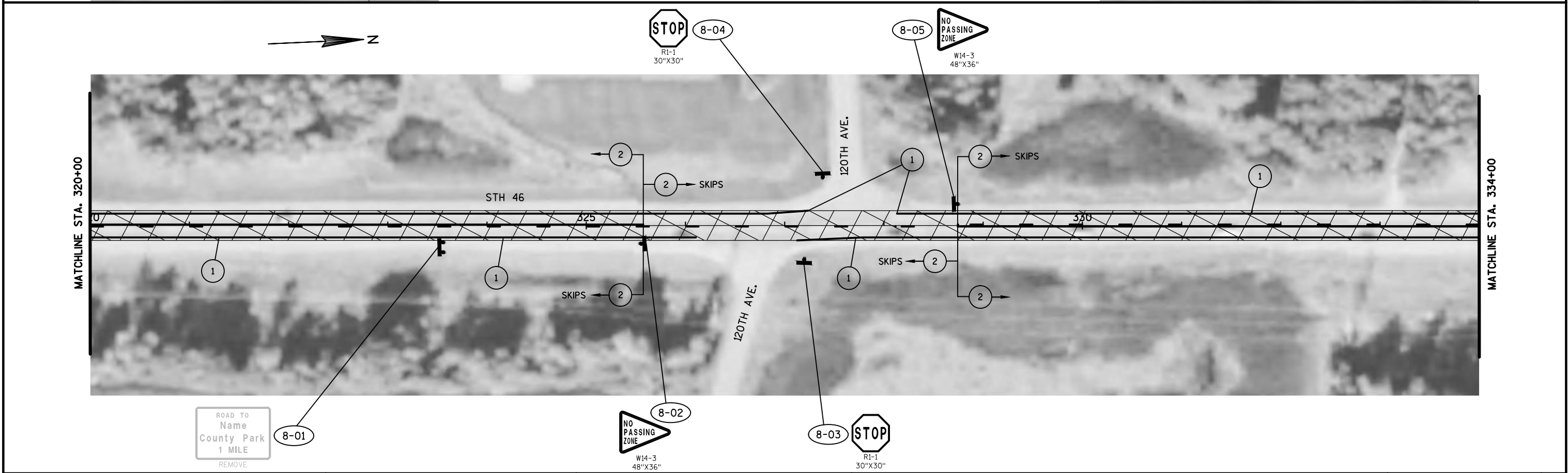
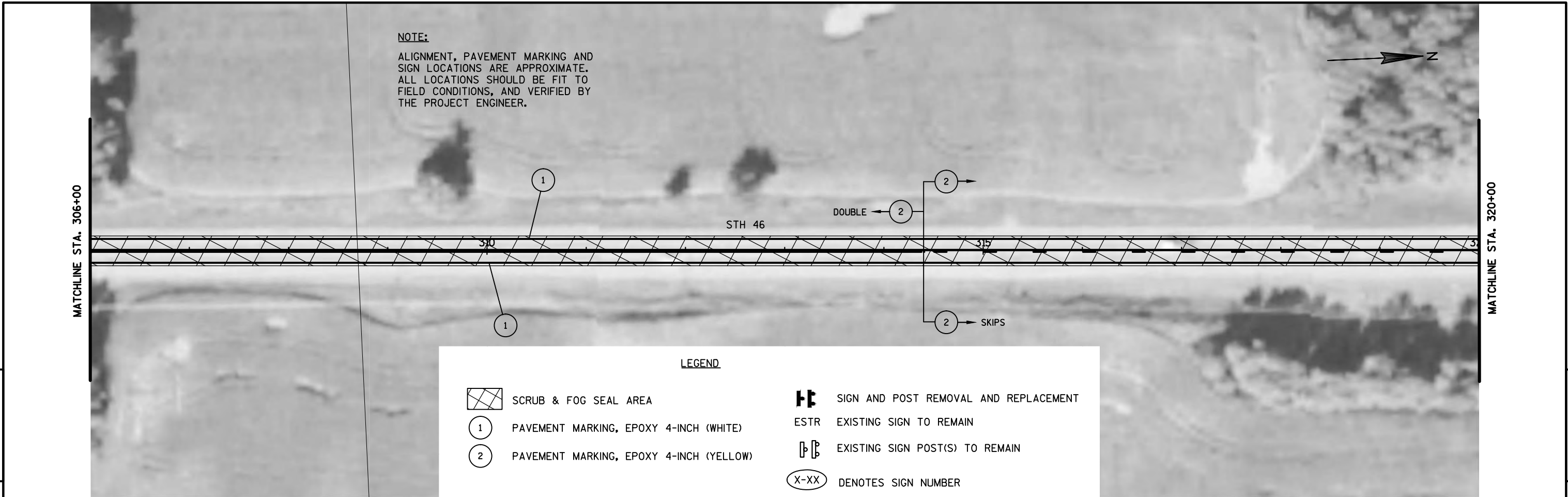
COUNTY: POLK

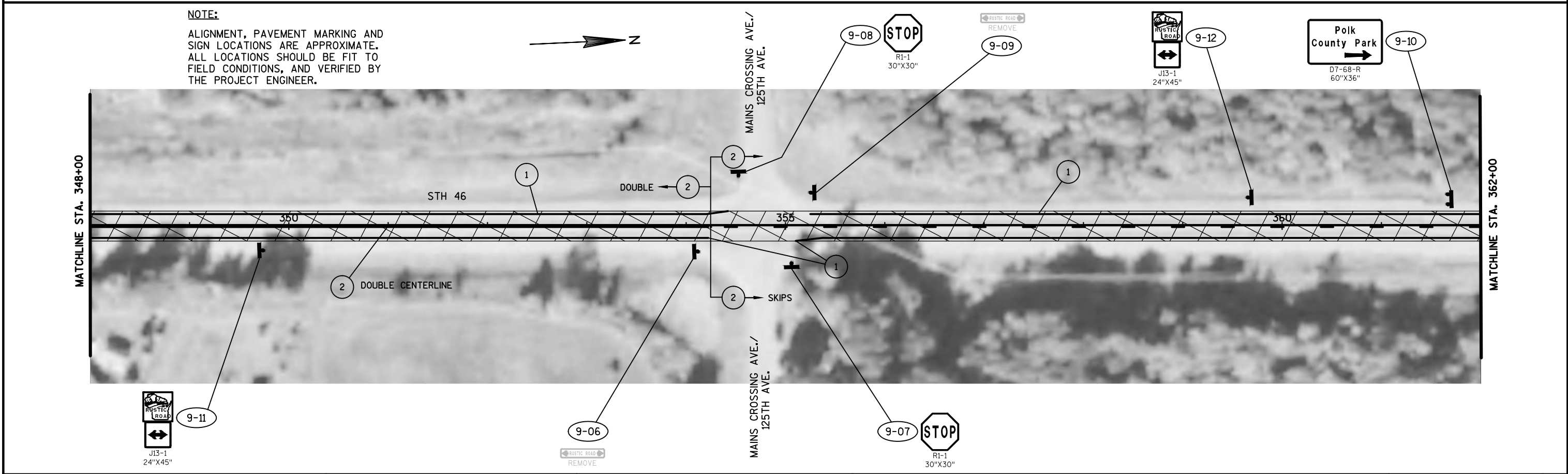
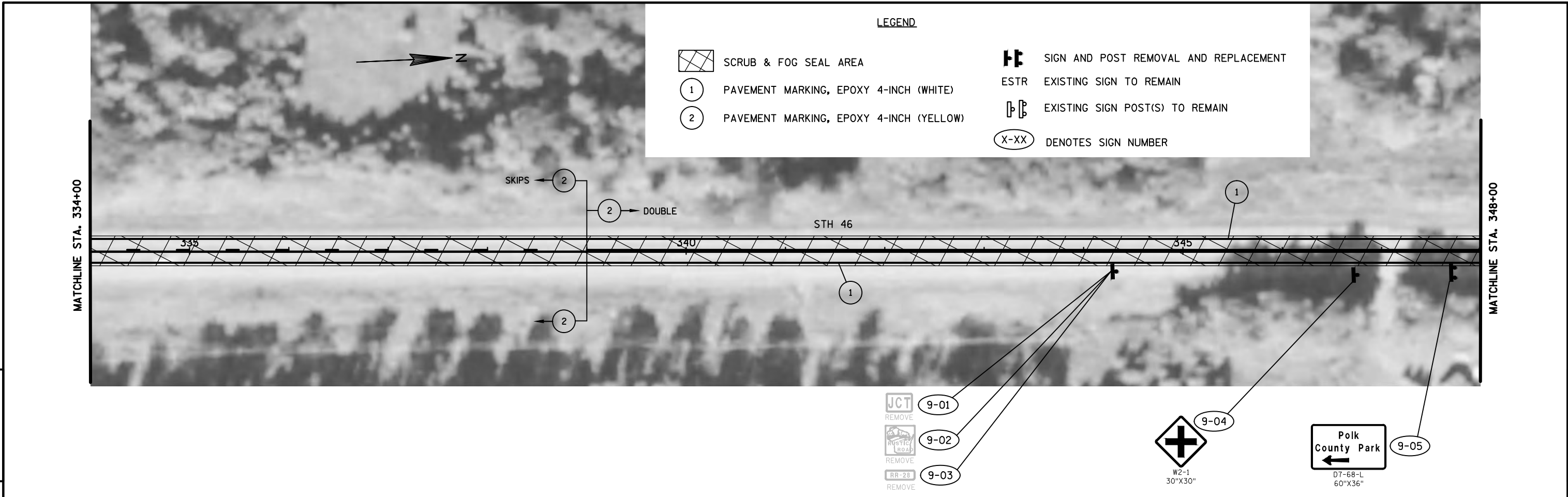
PLAN DETAILS

SHEET

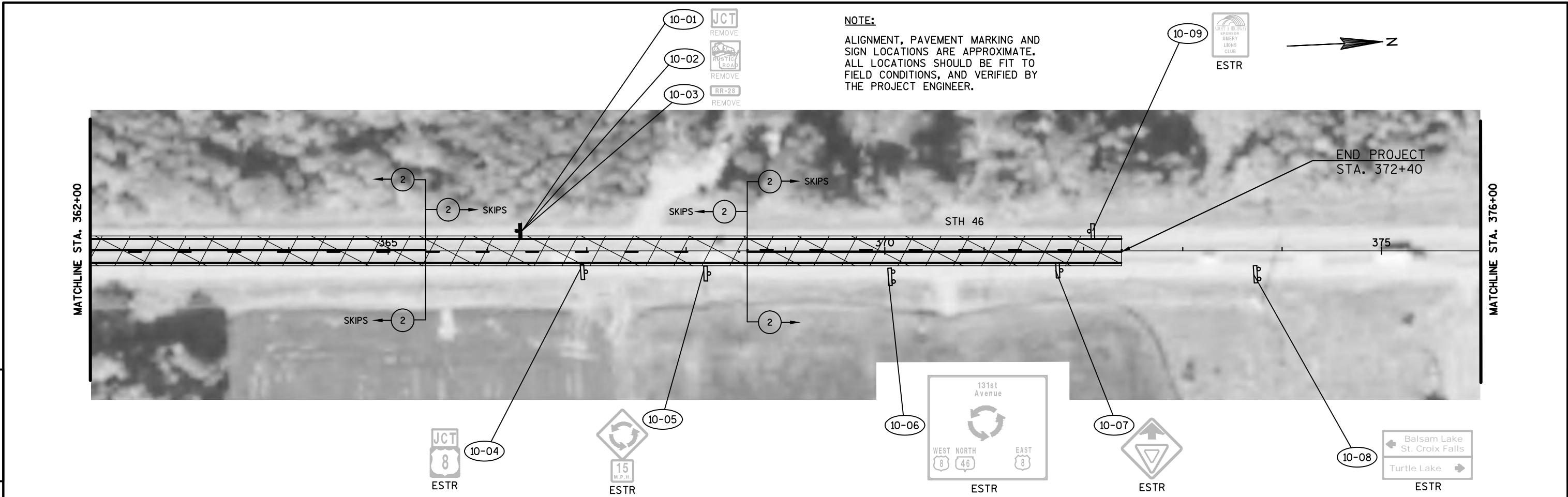
E



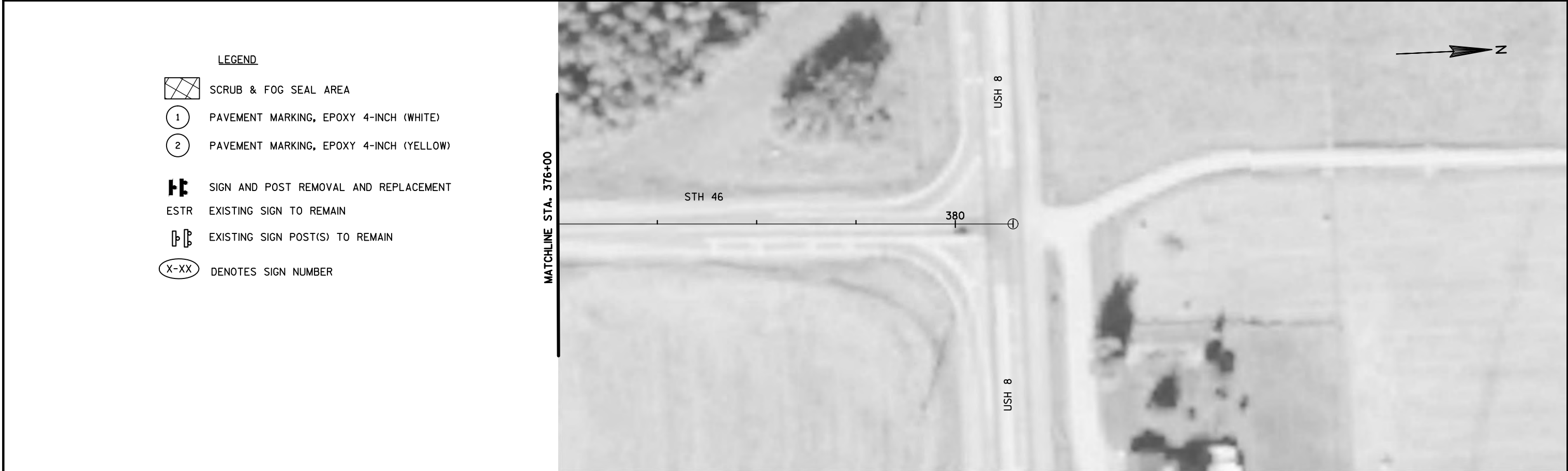




5



5



PROJECT NO: 8070-00-60	HWY: STH 46	COUNTY: POLK	PLAN DETAILS	SHEET	E
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NOTE:

ALIGNMENT AND PAVEMENT MARKING LOCATIONS ARE APPROXIMATE. ALL LOCATIONS SHOULD BE FIT TO FIELD CONDITIONS, AND VERIFIED BY THE PROJECT ENGINEER.

Cumberland 10

D2-1
90"X15"
1-02

WEST
48
J4-1
24"X36"
1-01

STOP
R1-1
30"X30"
1-03

SOUTH
25
J3-1
24"X57"
1-05

CUMBERLAND
BARRON
D1-2
84"X30"
1-08



MATCHLINE STA. 131+00

STH 48

120+00

125+00

130+00

LEGEND



SCRUB & FOG SEAL AREA



PAVEMENT MARKING, EPOXY 4-INCH (WHITE)



PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)



SIGN AND POST REMOVAL AND REPLACEMENT



EXISTING SIGN TO REMAIN



EXISTING SIGN POST(S) TO REMAIN



DENOTES SIGN NUMBER

SOUTH
25
J3-1
24"X57"
1-04

EAST
48
J4-1
24"X36"
1-07

Rice Lake 6
D2-1
72"X15"
1-06

15TH STREET

SKIPS
1

2

STH 48

SKIPS
2

1

1

BEGIN PROJECT
STA. 121+50

STH 25

5

5

SOUTH WEST
25 48
J2-2
48"X57"
1-09

JCT
25
J1-1
24"X39"
1-10

NO PASSING ZONE
W14-3
48"X36"
1-11



MATCHLINE STA. 145+00

MATCHLINE STA. 131+00

135+00

140+00

145+00

SKIPS

DOUBLE

1

STH 48

DOUBLE

2

SKIPS

1

2

NO PASSING ZONE
W14-3
48"X36"
1-12

PROJECT NO: 8120-00-60

HWY: STH 48

COUNTY: BARRON

PLAN DETAILS

SHEET

E

FILE NAME : G:\WDOTCON\14023 (2014 - 2016 DESIGN WORK ORDERS)\14023-018\CIVIL 3D\SHEETSPLAN\SIGNING & MARKING\023201-PS_STH 48.DWG

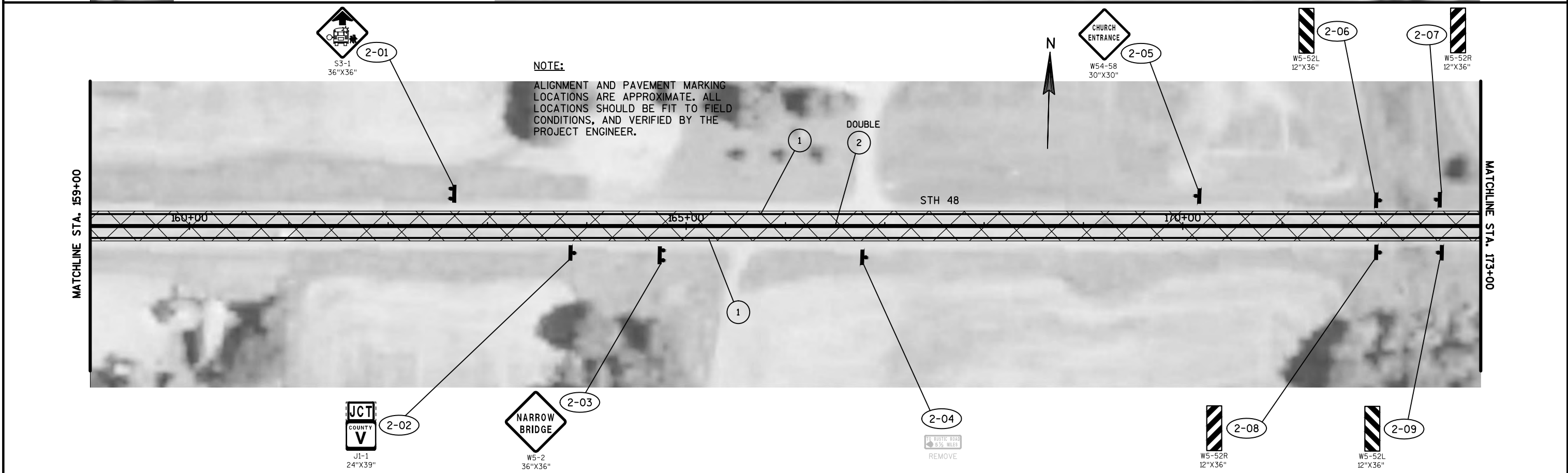
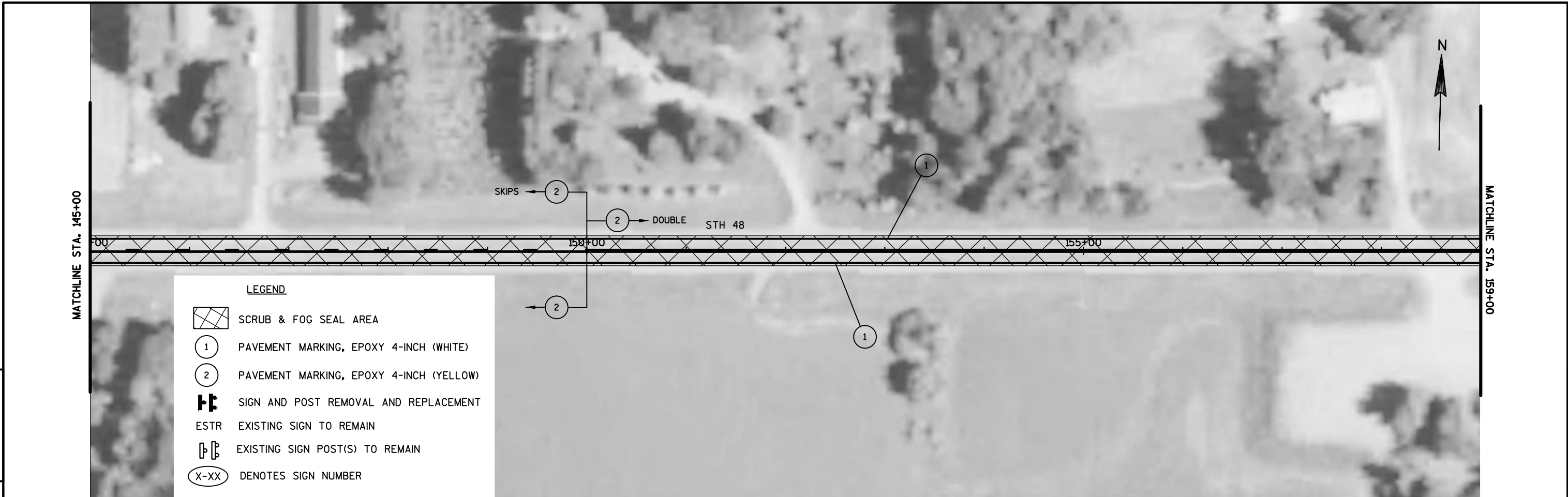
PLOT DATE : 10/27/2016 4:06 PM

PLOT BY : BRAD CUNNINGHAM

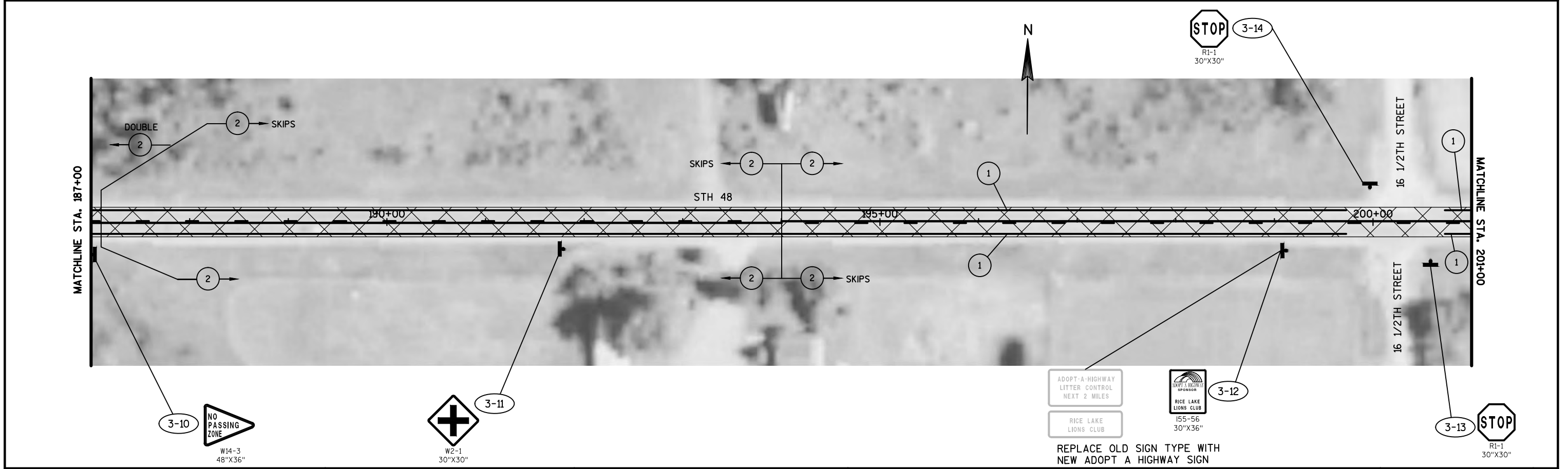
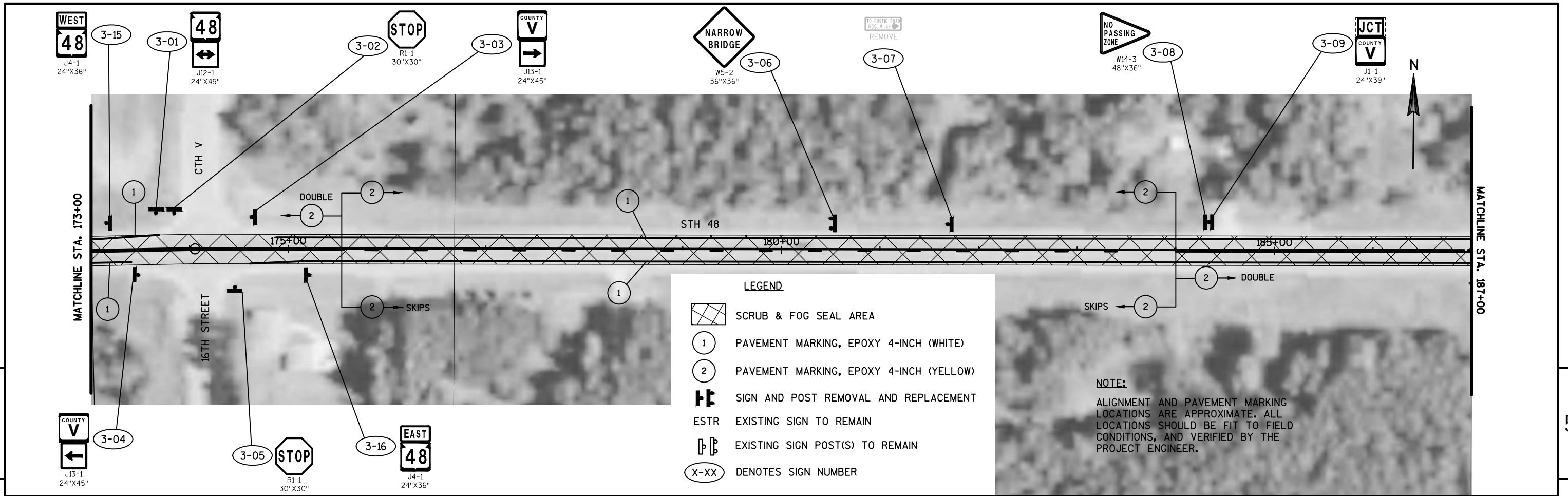
PLOT NAME :

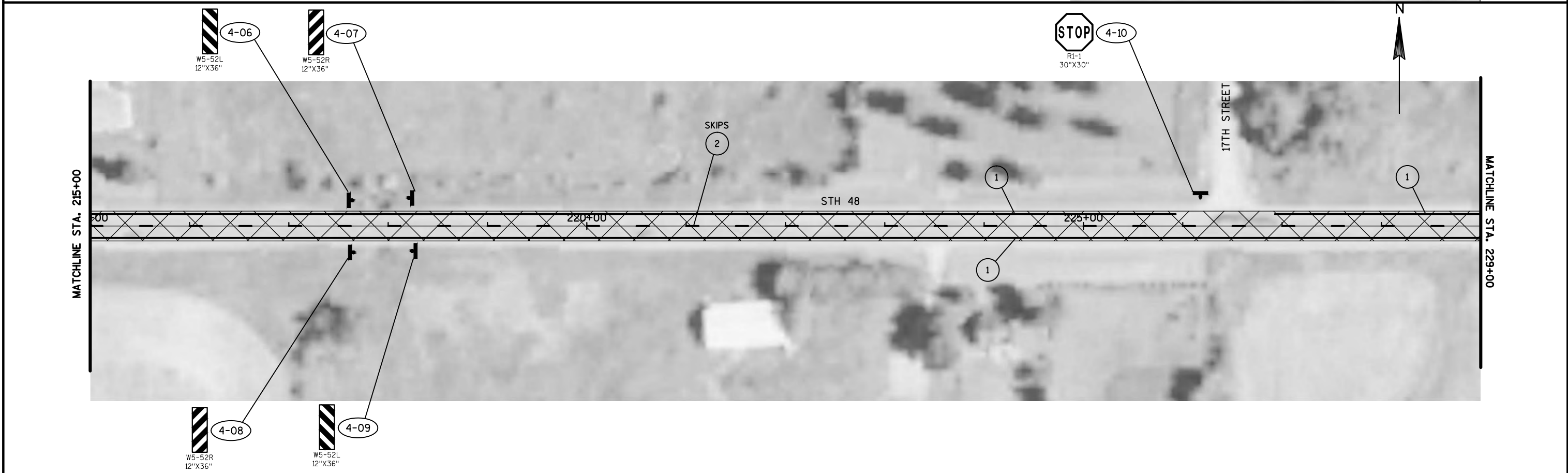
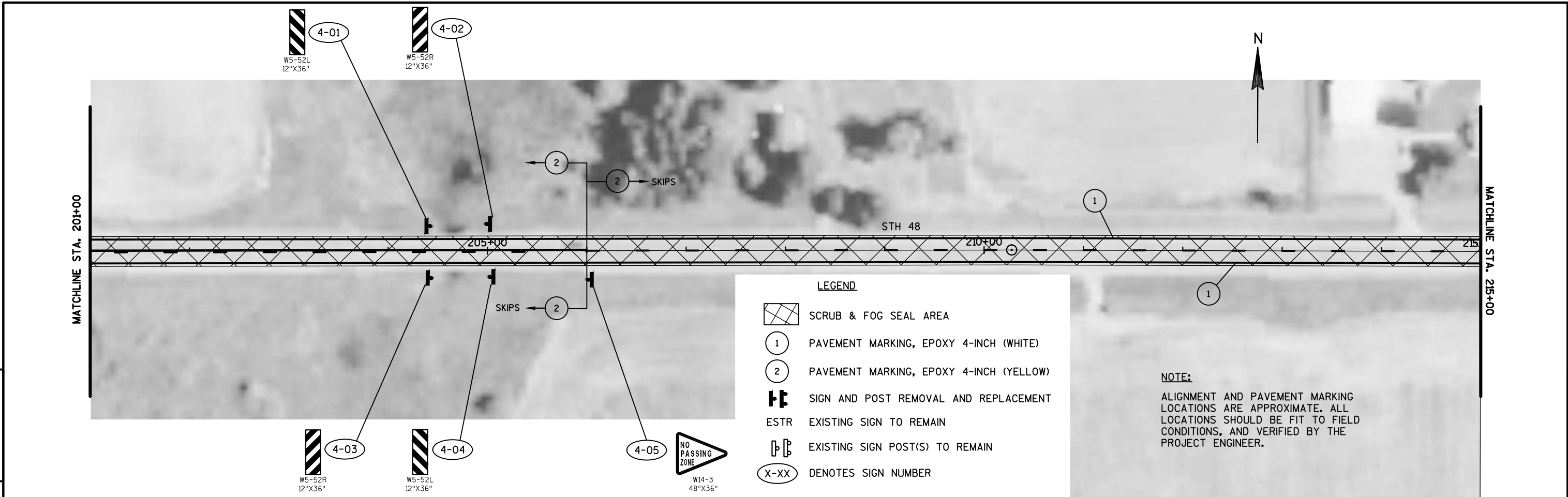
PLOT SCALE : 1 IN:100 FT

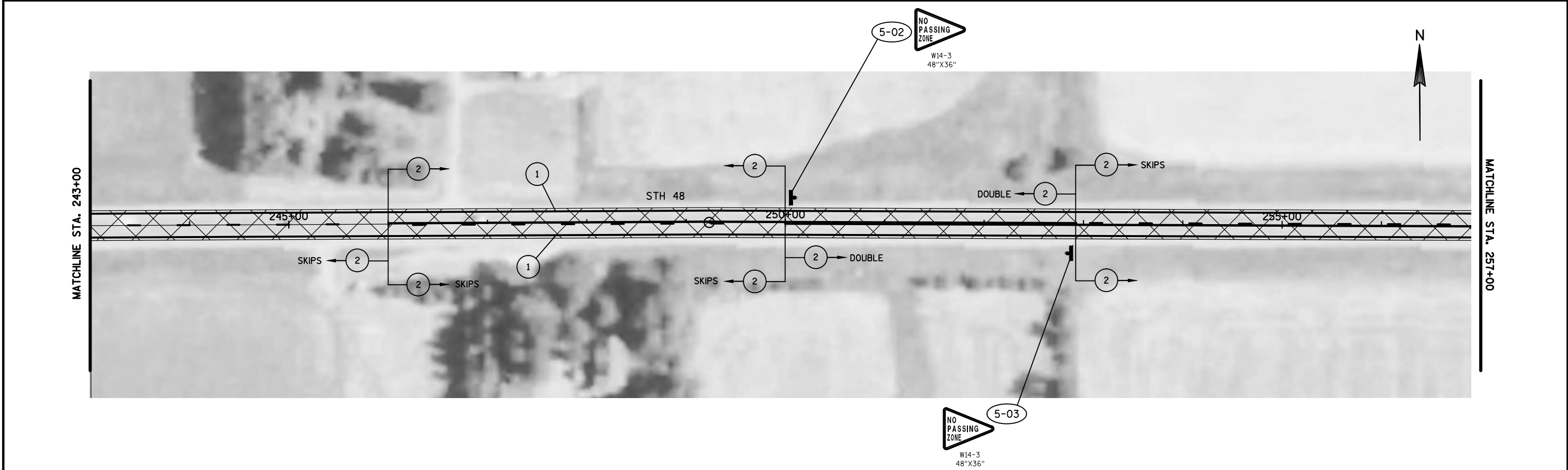
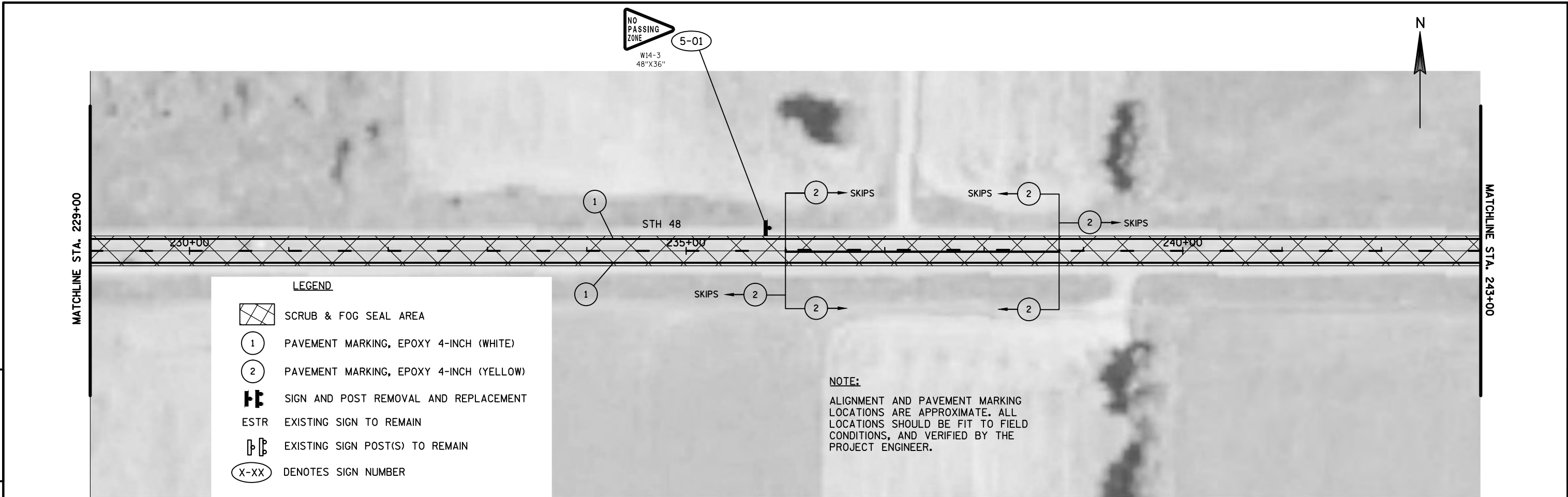
WISDOT/CADDs SHEET 44

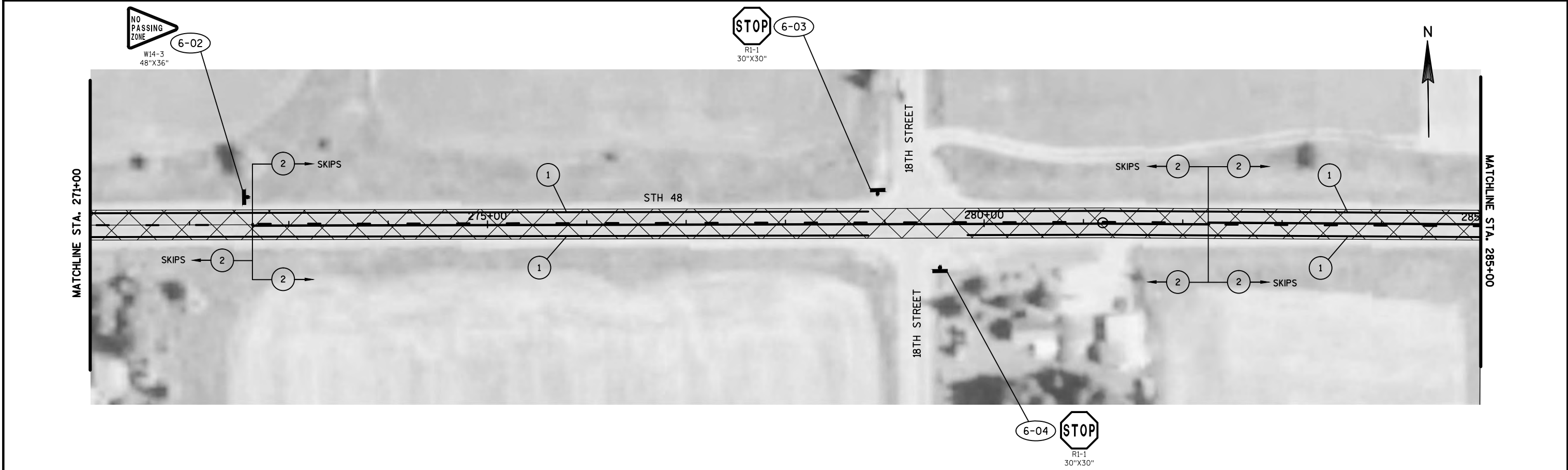
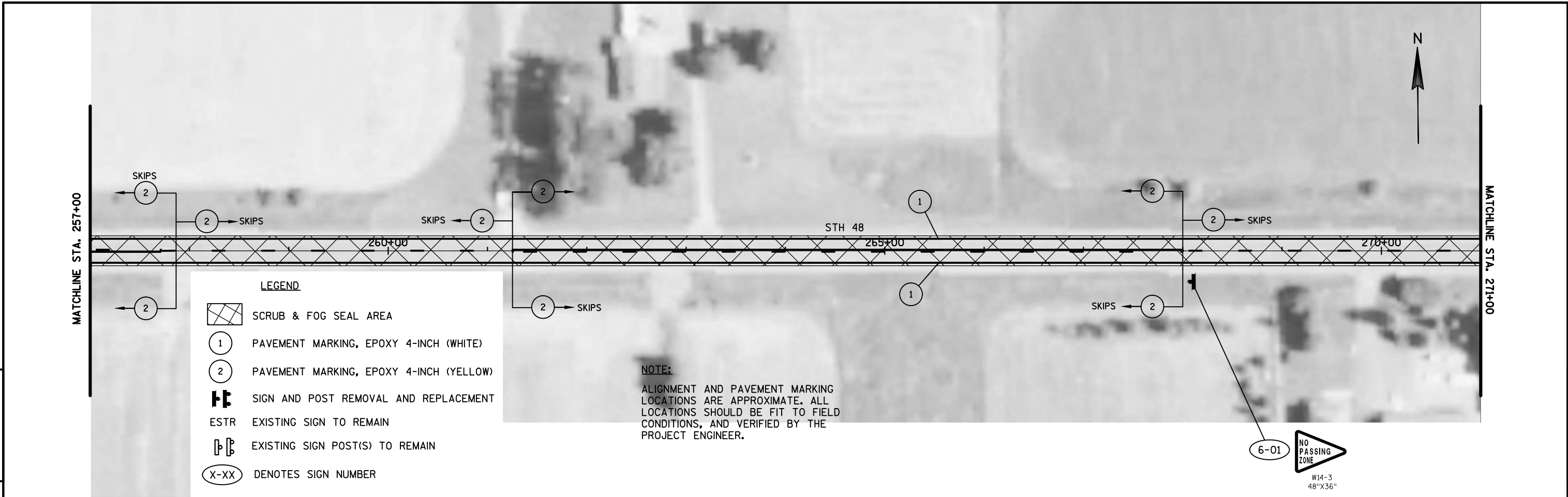


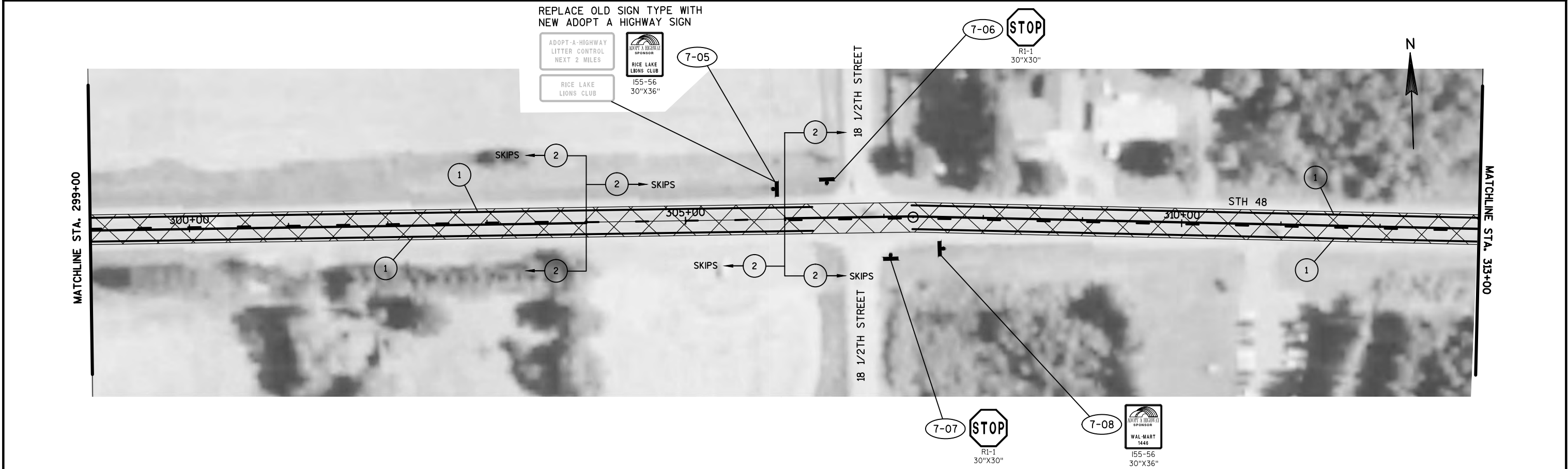
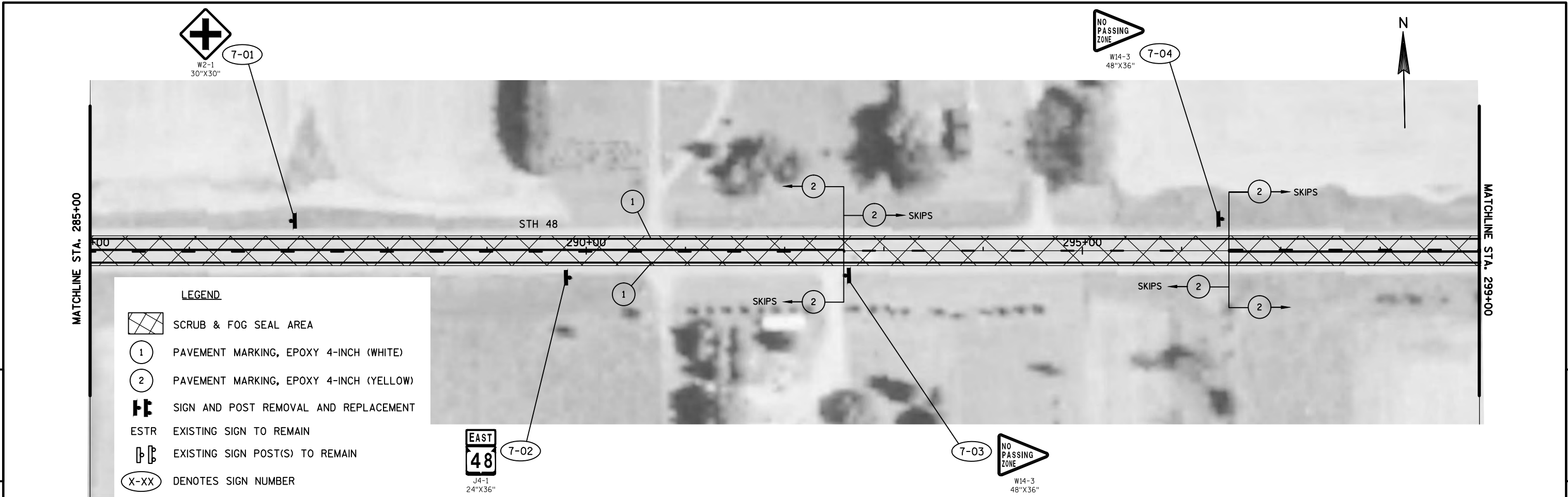
PROJECT NO: 8120-00-60	HWY: STH 48	COUNTY: BARRON	PLAN DETAILS	SHEET	E
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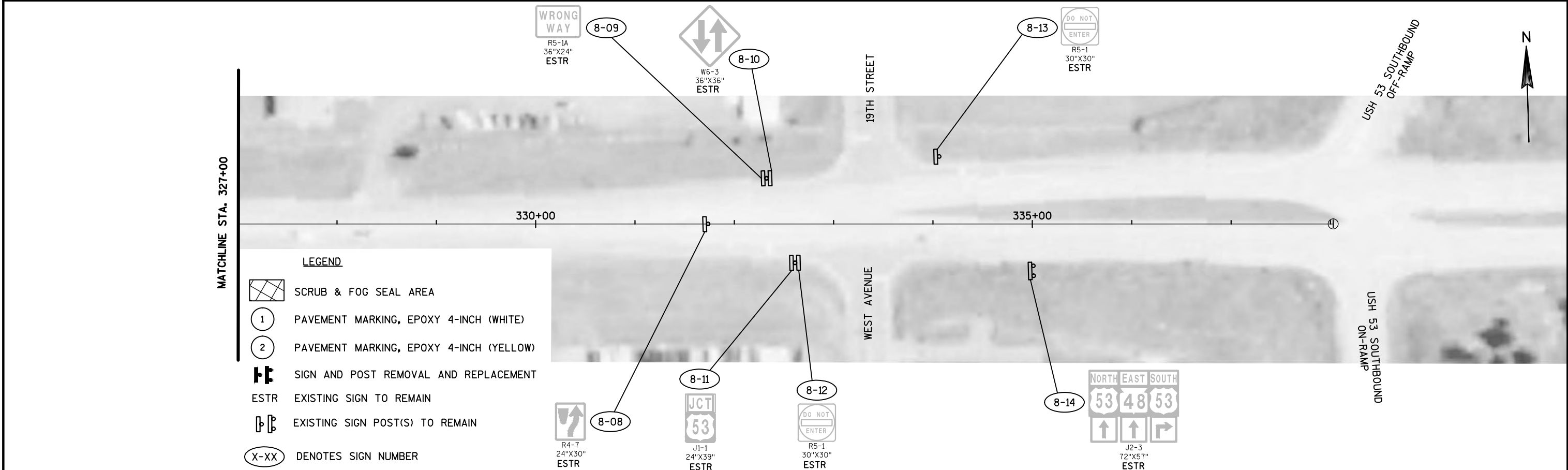
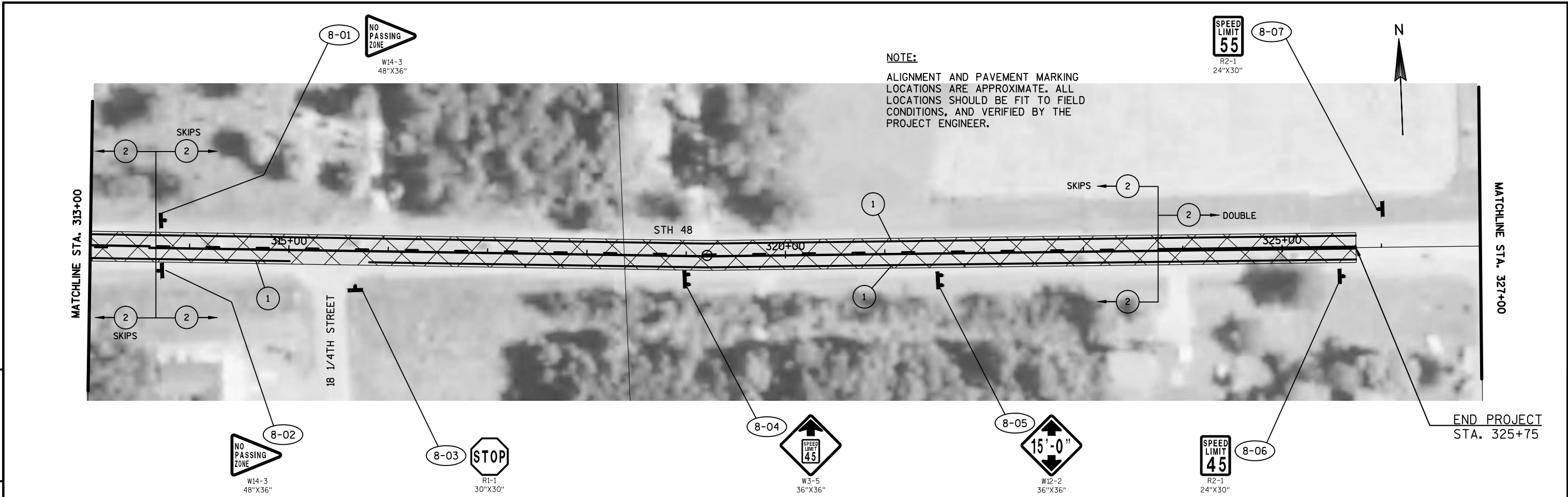












NOTE:

ALIGNMENT AND PAVEMENT MARKING LOCATIONS ARE APPROXIMATE. ALL LOCATIONS SHOULD BE FIT TO FIELD CONDITIONS, AND VERIFIED BY THE PROJECT ENGINEER.

LEGEND



SCRUB & FOG SEAL AREA

1

PAVEMENT MARKING, EPOXY 4-INCH (WHITE)

2

PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)



LANDELA RD.

CTH F

MATCHLINE STA. 130+00

BEGIN PROJECT
STA. 126+00

2 → SKIPS

1

2

1

5

5

MATCHLINE STA. 130+00

1

2 → SKIPS

2

DOUBLE

135+00

2

STH 13

140+00

1



MATCHLINE STA. 135+00

PROJECT NO: 8510-01-60

HWY: STH 13

COUNTY: DOUGLAS

PLAN DETAILS

SHEET

E

FILE NAME : G:\WDOTCO\14023 (2014 - 2016 DESIGN WORK ORDERS)\14023-018\CIVIL 3D\SHEETS\PLAN\SIGNING & MARKING\023201-PS_STH 13.DWG
LAYOUT NAME - SHEET - (1)

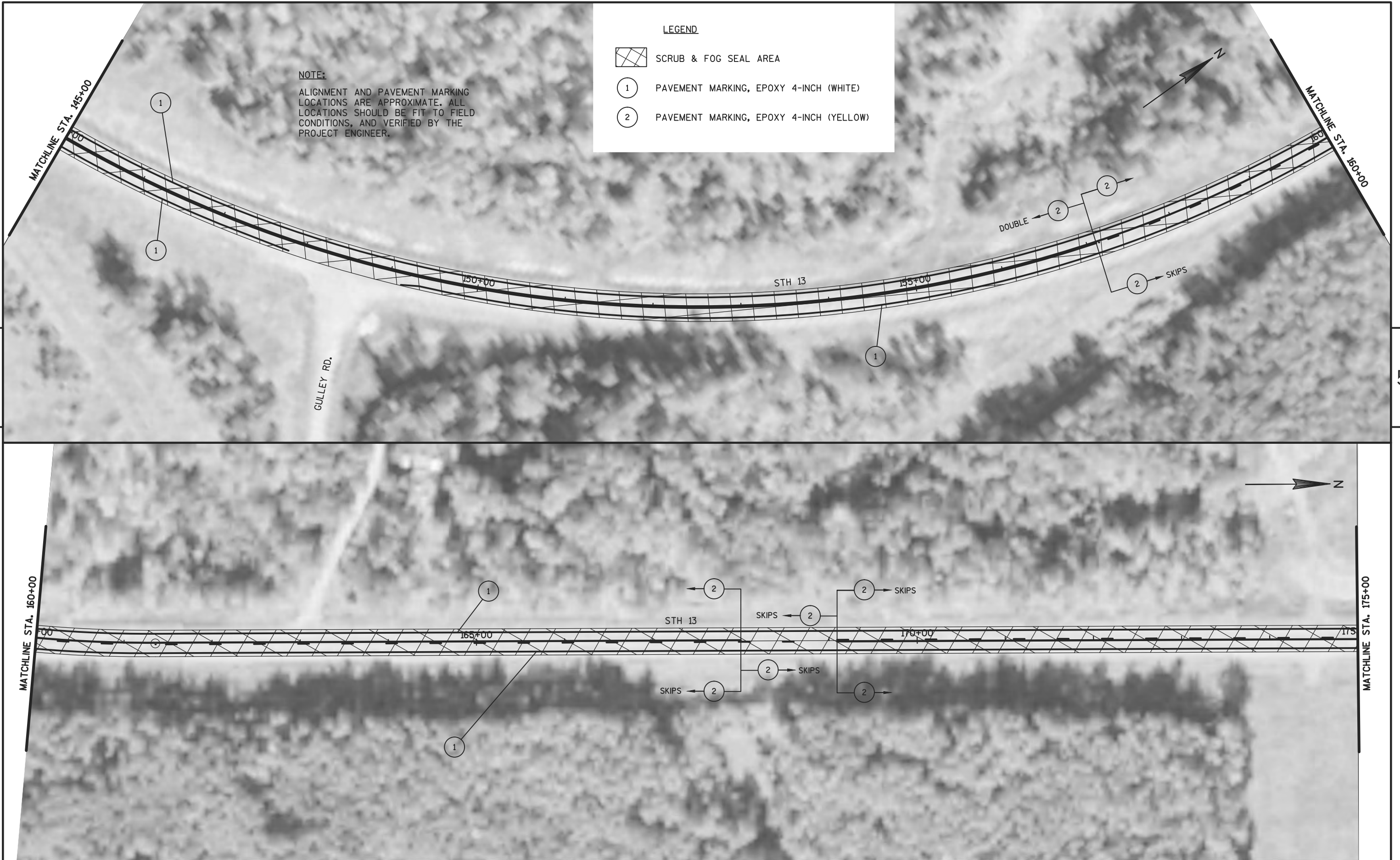
PLOT DATE : 9/28/2016 3:00 PM

PLOT BY : JENNIFER LAUER

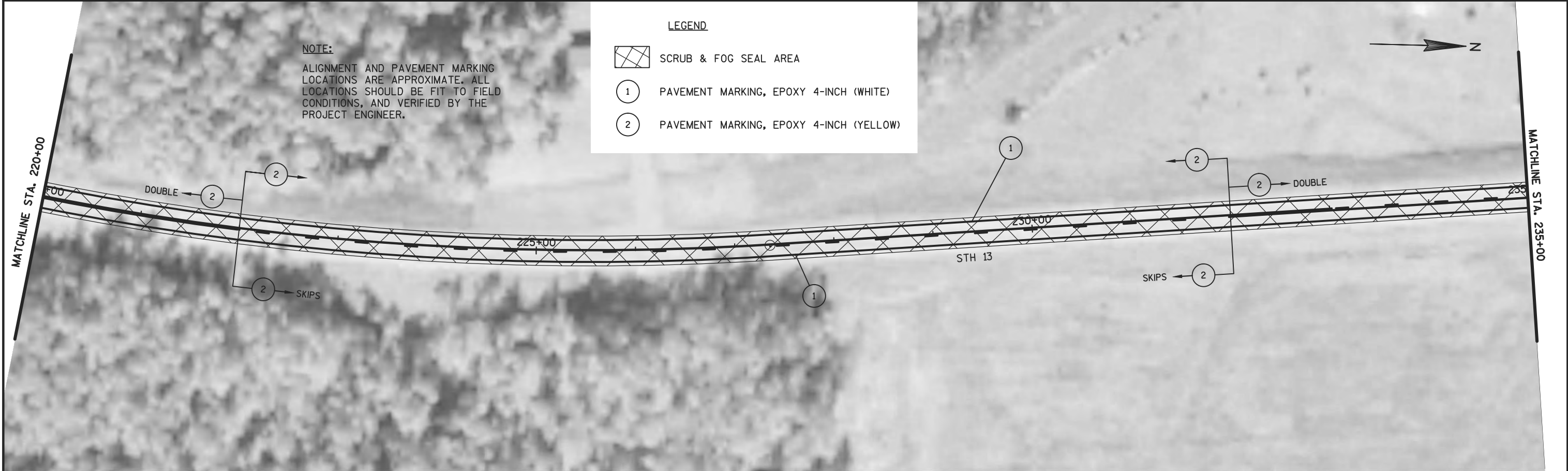
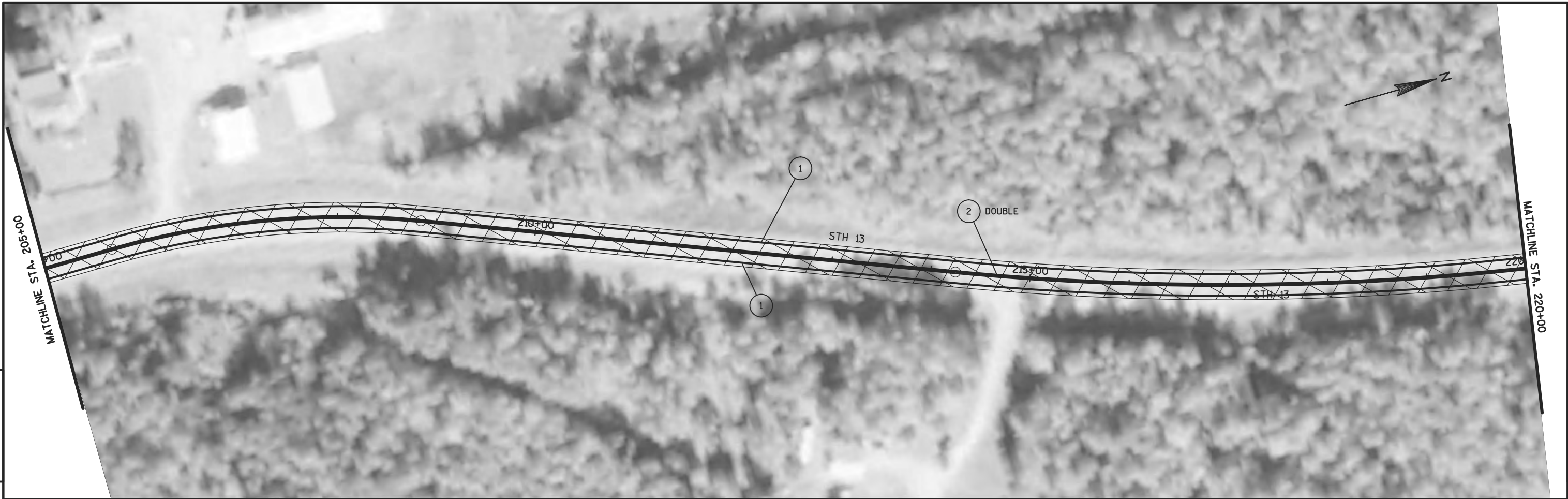
PLOT NAME :

PLOT SCALE : *****

WISDOT/CADDs SHEET 44







NOTE:
ALIGNMENT AND PAVEMENT MARKING
LOCATIONS ARE APPROXIMATE. ALL
LOCATIONS SHOULD BE FIT TO FIELD
CONDITIONS, AND VERIFIED BY THE
PROJECT ENGINEER.

LEGEND



SCRUB & FOG SEAL AREA



PAVEMENT MARKING, EPOXY 4-INCH (WHITE)



PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)

PROJECT NO: 8510-01-60

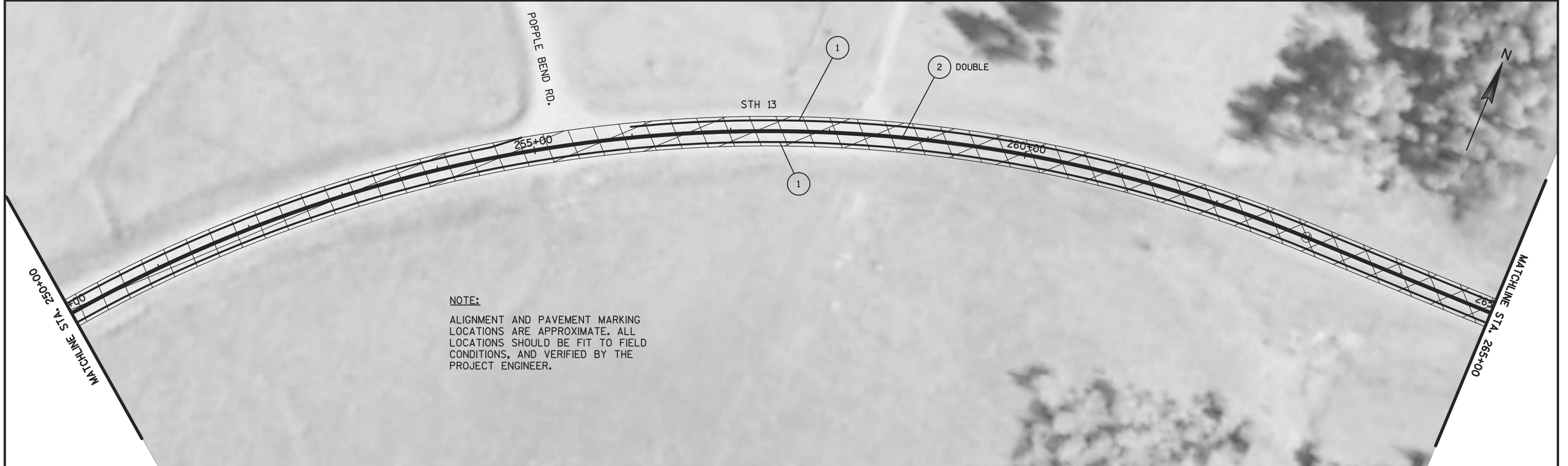
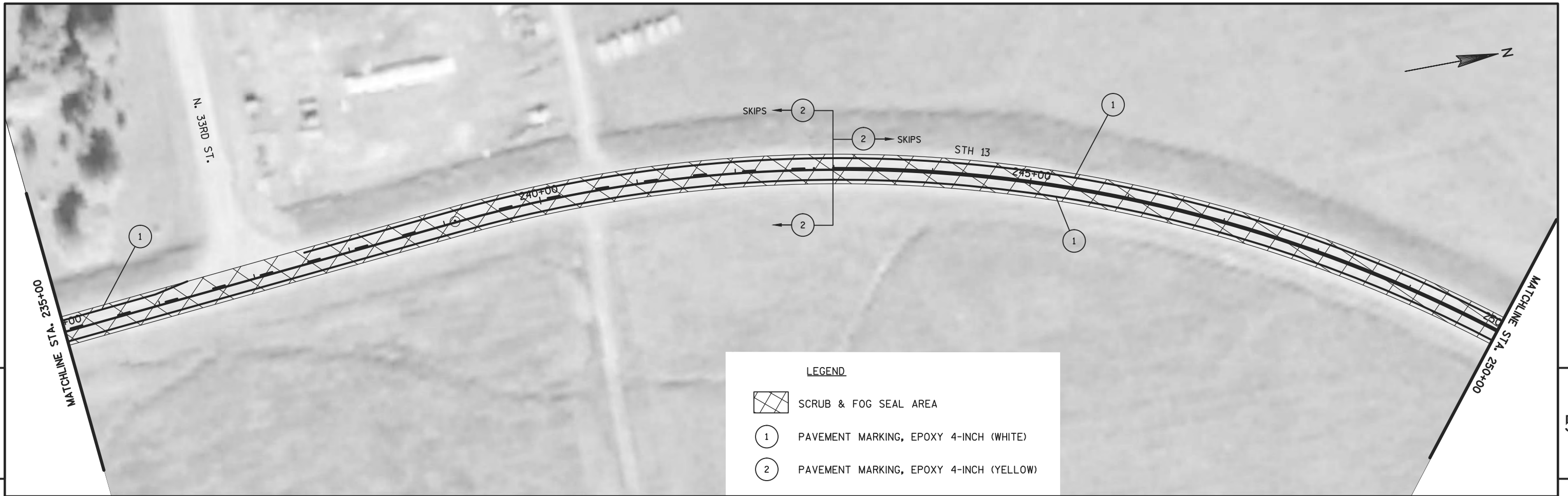
HWY: STH 13

COUNTY: DOUGLAS

PLAN DETAILS

SHEET

E





NOTE:
ALIGNMENT AND PAVEMENT MARKING
LOCATIONS ARE APPROXIMATE. ALL
LOCATIONS SHOULD BE FIT TO FIELD
CONDITIONS, AND VERIFIED BY THE
PROJECT ENGINEER.

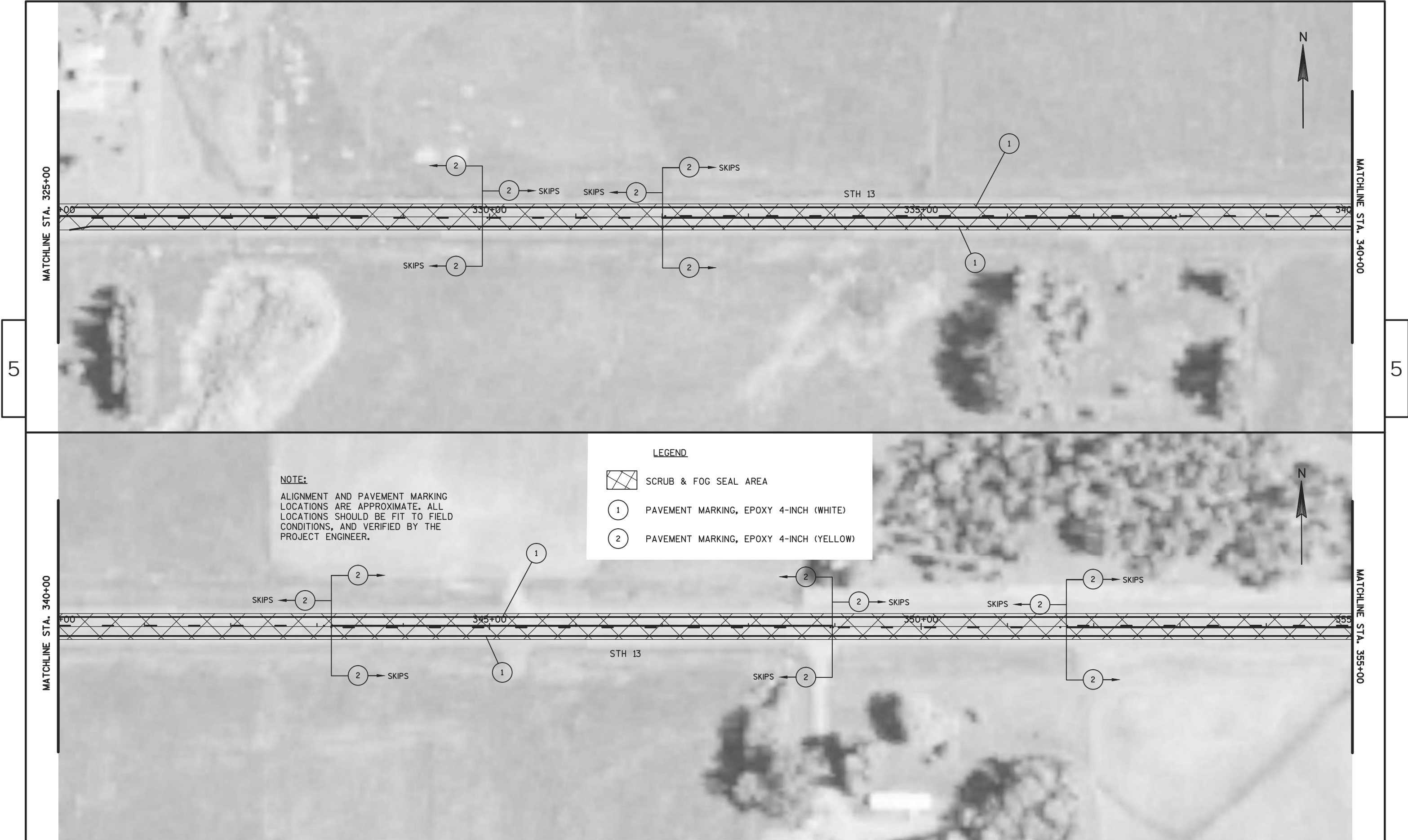
LEGEND

SCRUB & FOG SEAL AREA

1 PAVEMENT MARKING, EPOXY 4-INCH (WHITE)

2 PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)





PROJECT NO:8510-01-60

HWY:STH 13

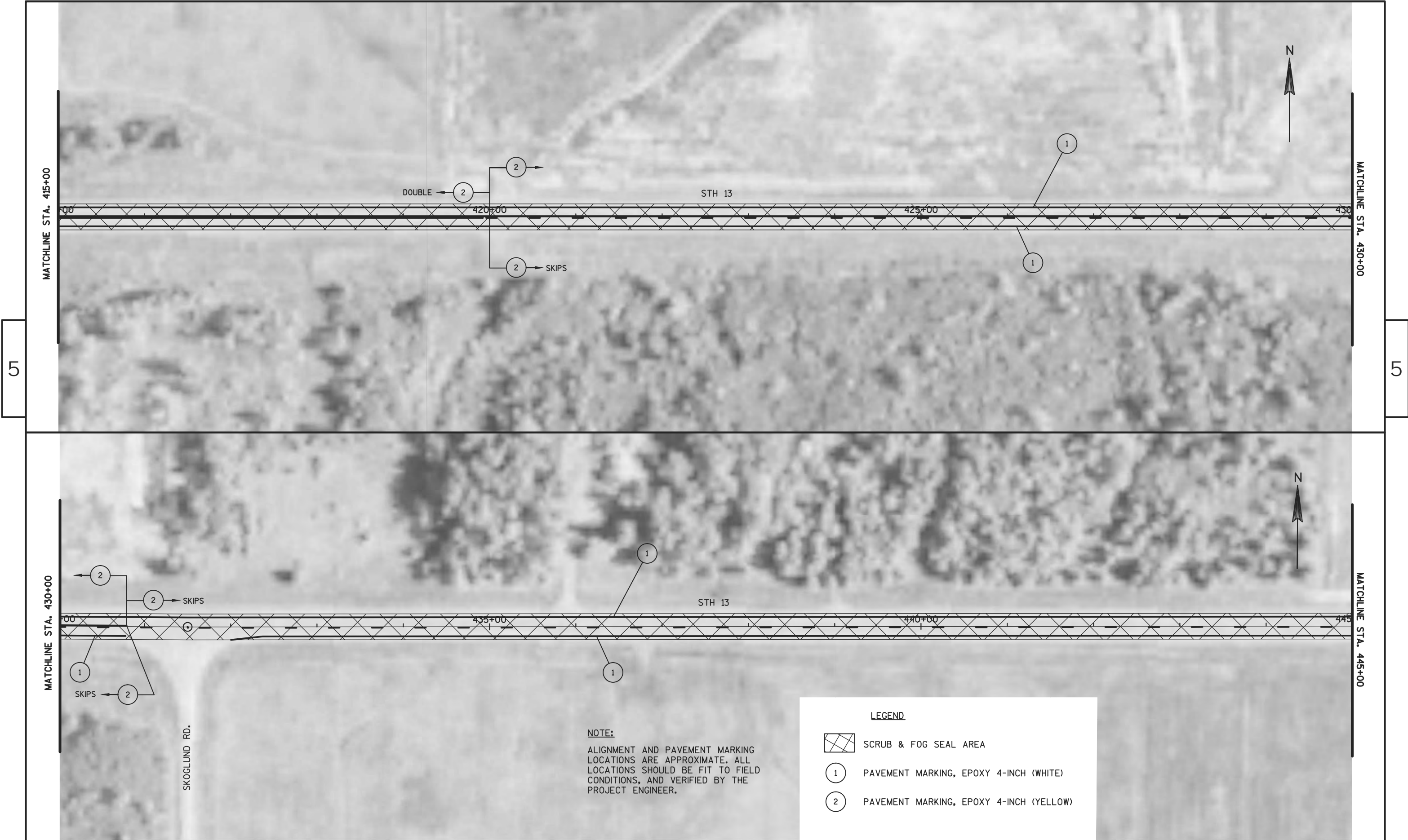
COUNTY:DOUGLAS

PLAN DETAILS

SHEET

E






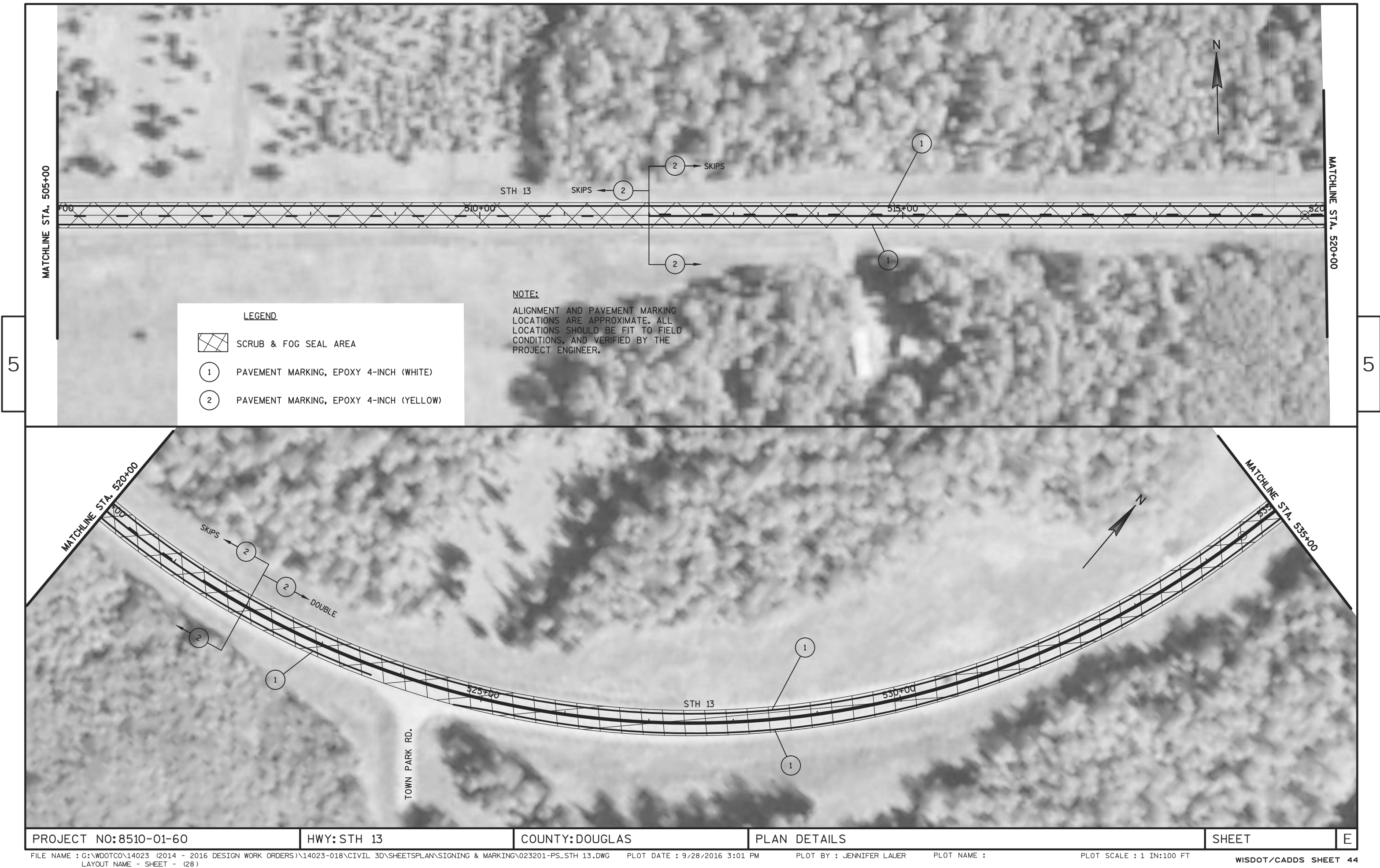


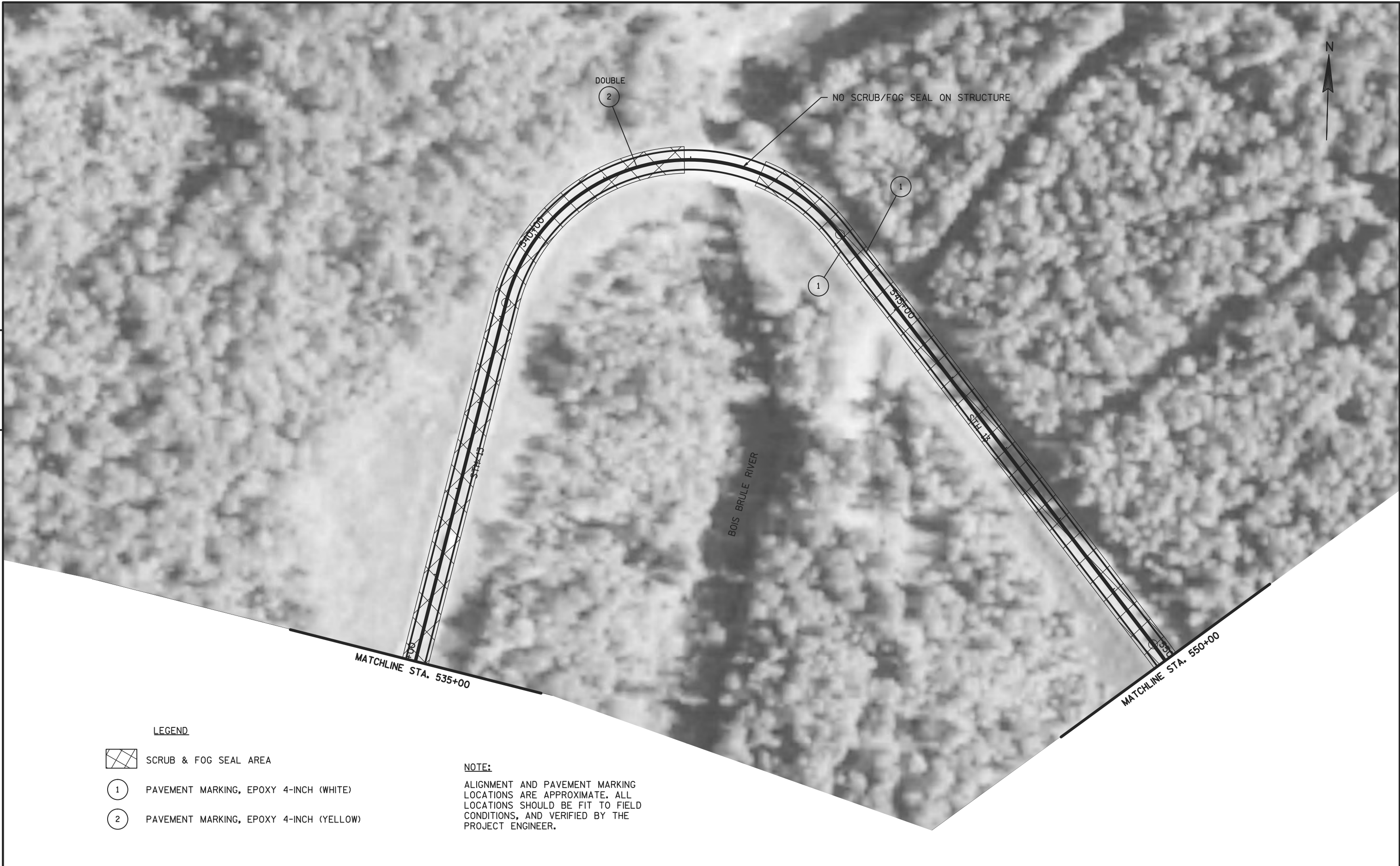


NOTE:
ALIGNMENT AND PAVEMENT MARKING
LOCATIONS ARE APPROXIMATE. ALL
LOCATIONS SHOULD BE FIT TO FIELD
CONDITIONS, AND VERIFIED BY THE
PROJECT ENGINEER.

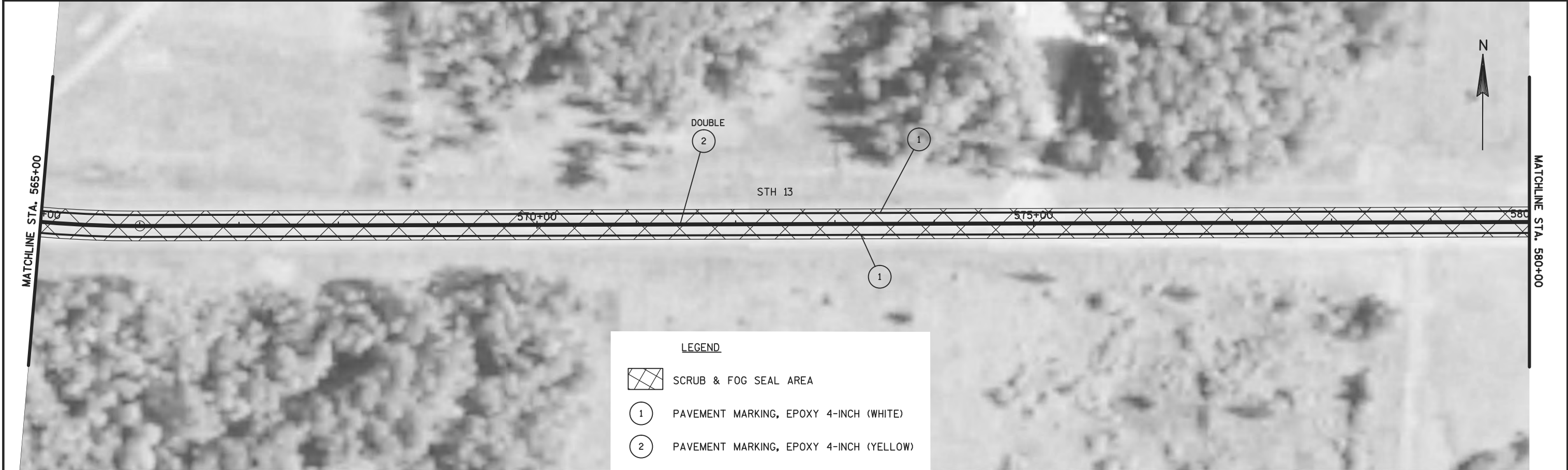
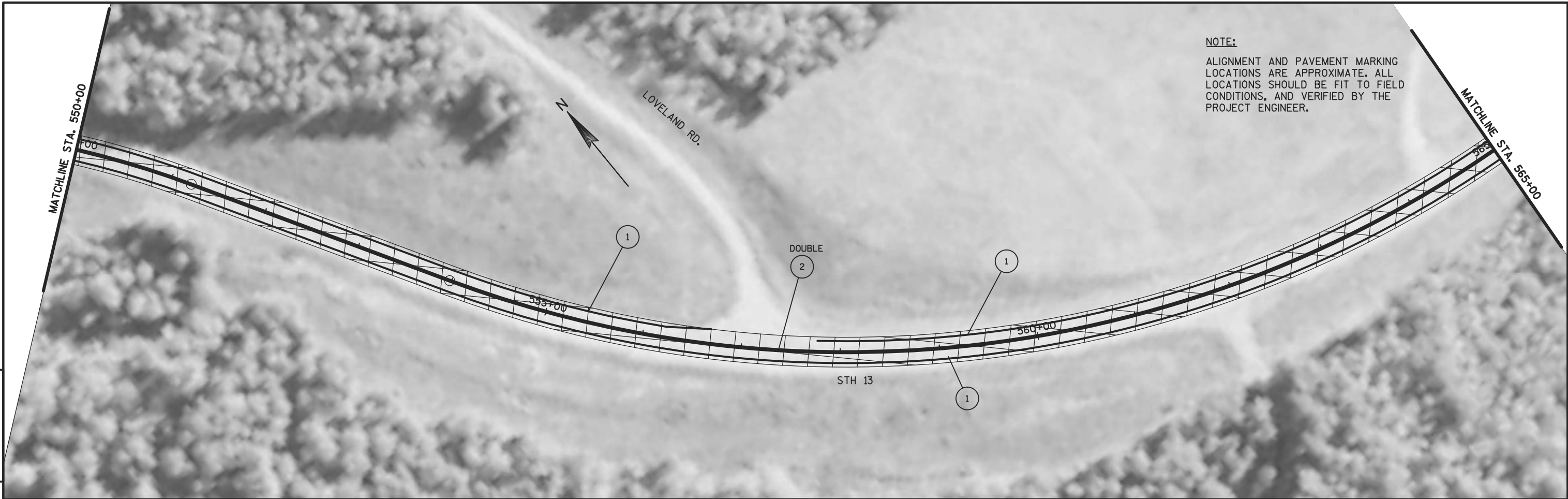
LEGEND

-  SCRUB & FOG SEAL AREA
-  PAVEMENT MARKING, EPOXY 4-INCH (WHITE)
-  PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)

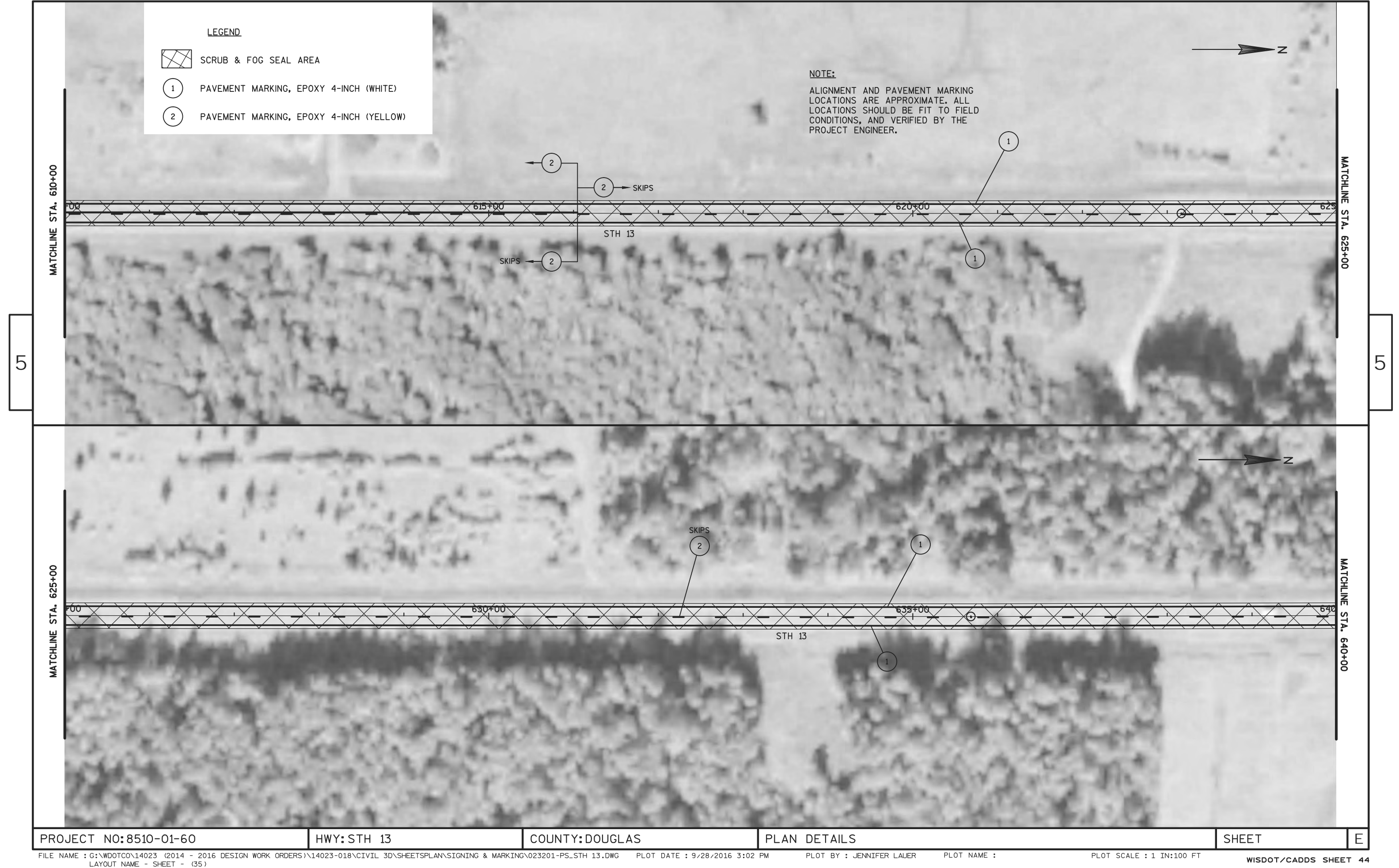




PROJECT NO:8510-01-60	HWY:STH 13	COUNTY:DOUGLAS	SIGNING & MARKING	SHEET	E
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PROJECT NO: 8510-01-60

HWY: STH 13

COUNTY: DOUGLAS

PLAN DETAILS

SHEET

E



PROJECT NO:8510-01-60

HWY:STH 13

COUNTY:DOUGLAS

PLAN DETAILS

SHEET

E



5

5

PROJECT NO:8510-01-60

HWY:STH 13

COUNTY:DOUGLAS

PLAN DETAILS

SHEET

E



PROJECT NO:8510-01-60

HWY:STH 13

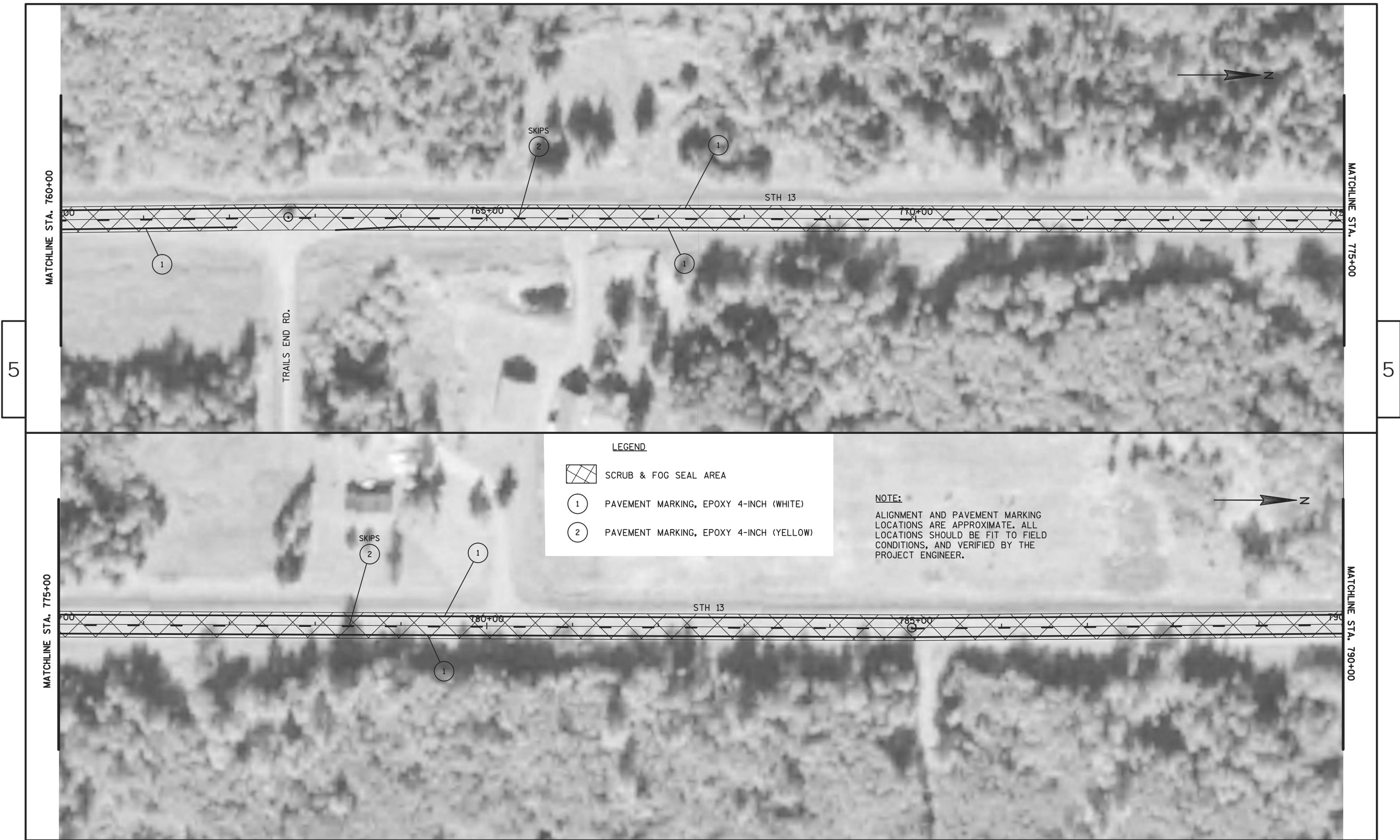
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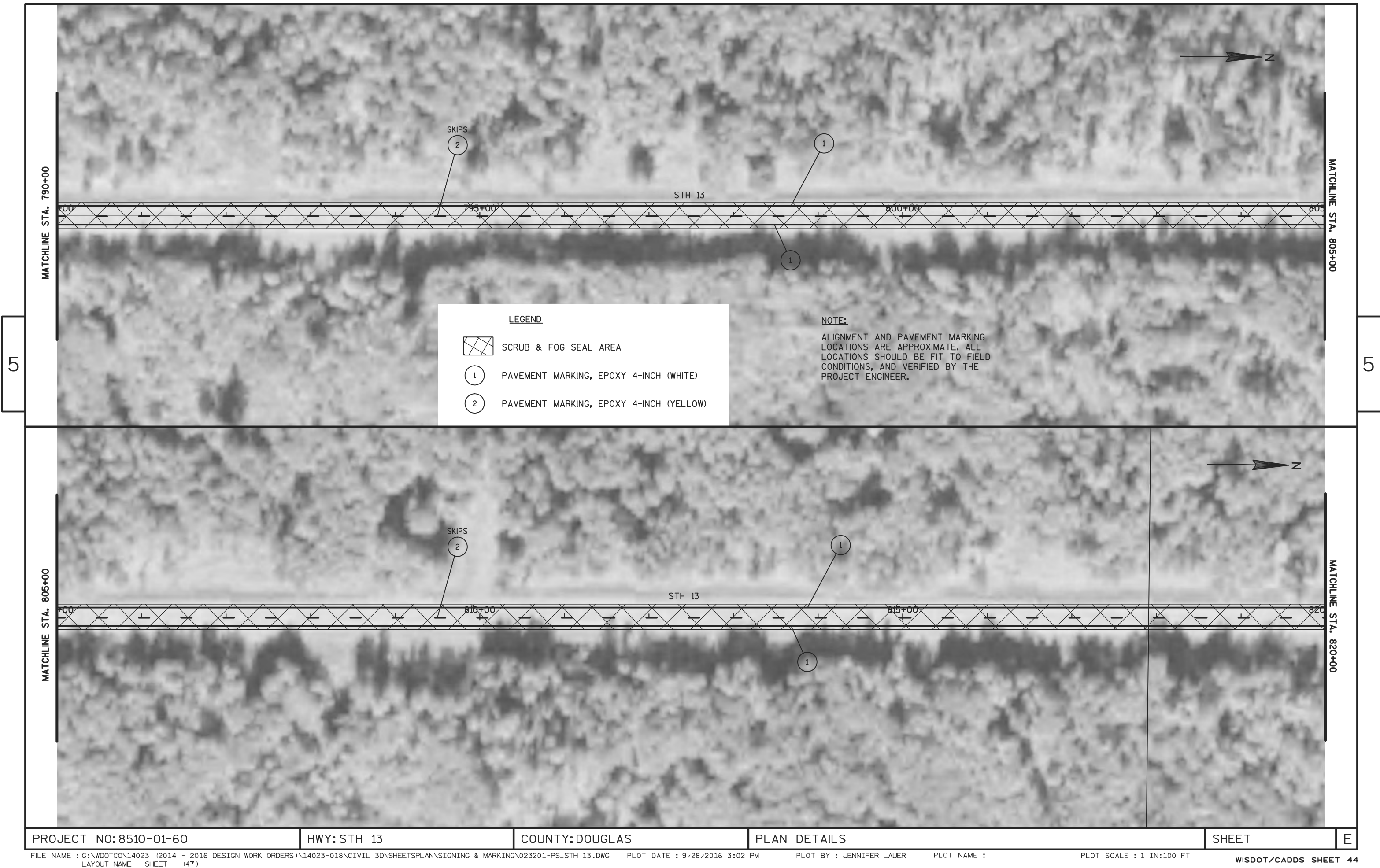
PLAN DETAILS

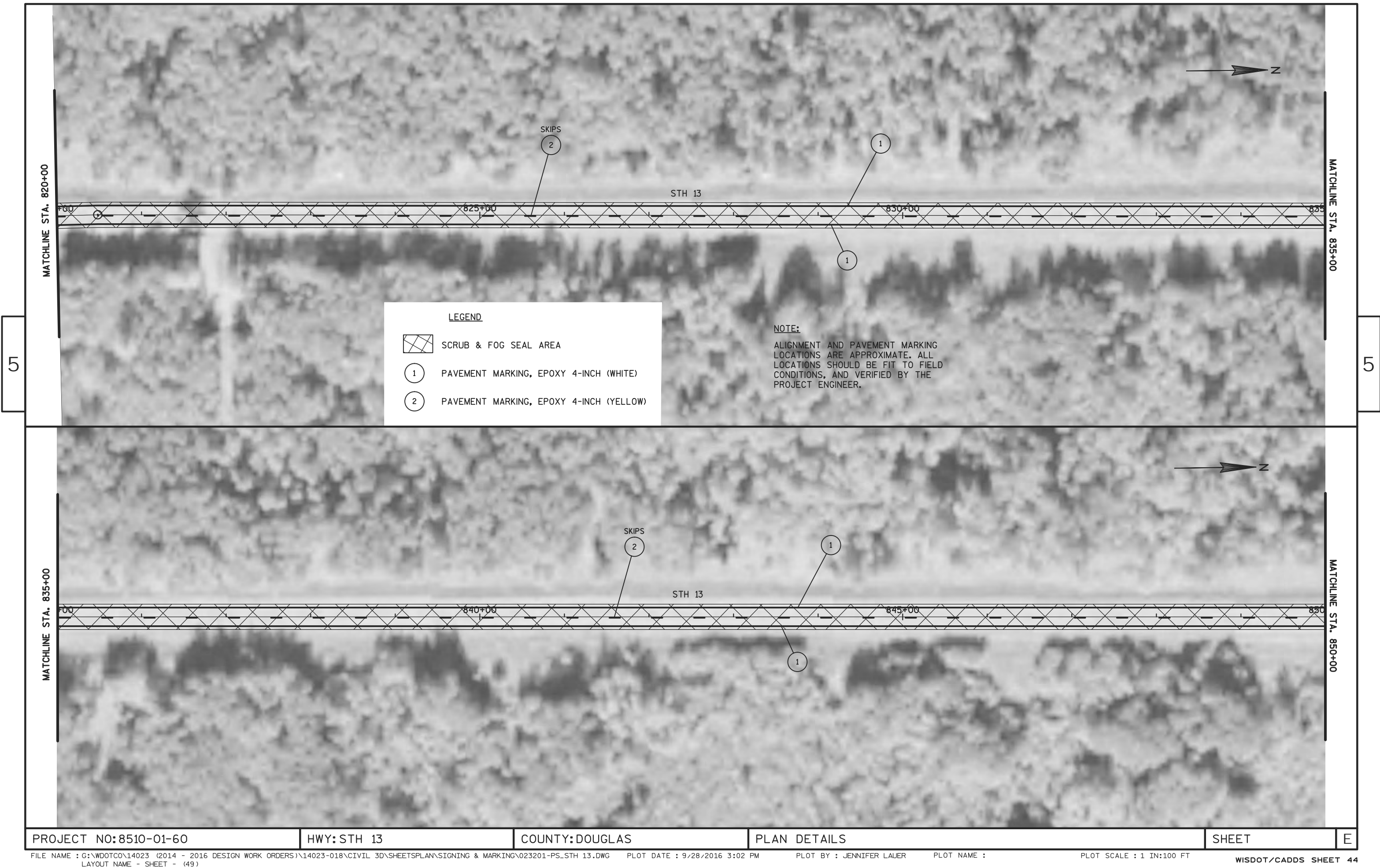
SHEET

E









PROJECT NO: 8510-01-60

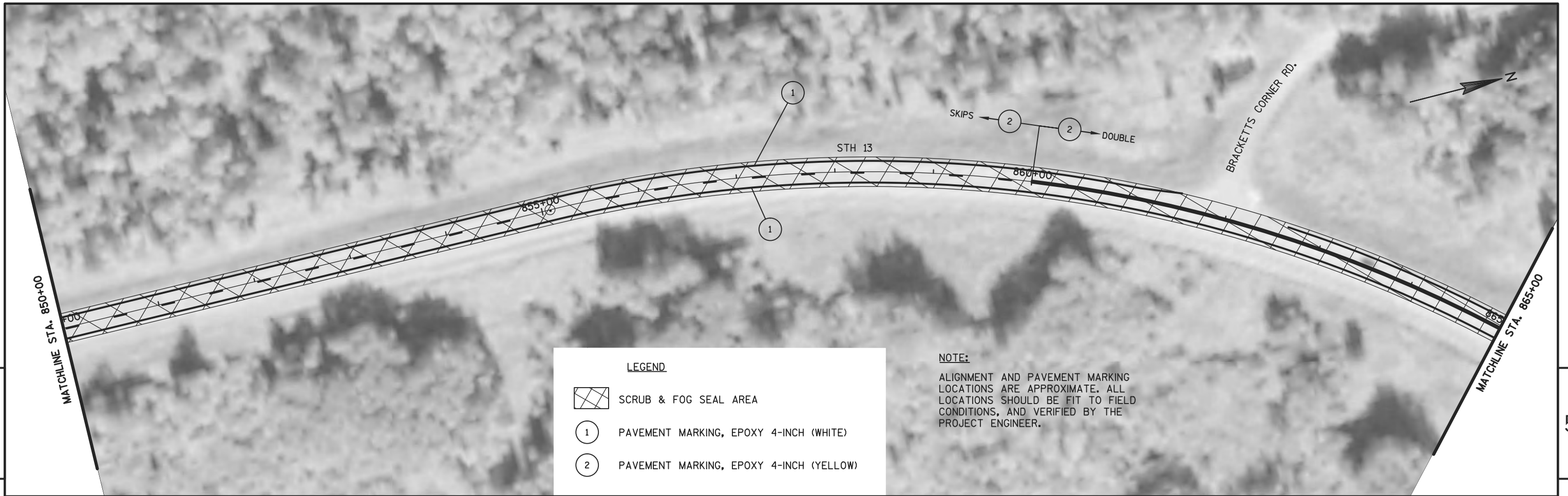
HWY: STH 13

COUNTY: DOUGLAS

PLAN DETAILS

SHEET

E



LEGEND

SCRUB & FOG SEAL AREA

1

PAVEMENT MARKING, EPOXY 4-INCH (WHITE)

2

PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)

NOTE:
ALIGNMENT AND PAVEMENT MARKING
LOCATIONS ARE APPROXIMATE. ALL
LOCATIONS SHOULD BE FIT TO FIELD
CONDITIONS, AND VERIFIED BY THE
PROJECT ENGINEER.

PROJECT NO: 8510-01-60	HWY: STH 13	COUNTY: DOUGLAS	PLAN DETAILS	SHEET	E
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FILE NAME : G:\WDOTCO\14023 (2014 - 2016 DESIGN WORK ORDERS)\14023-018\CIVIL 3D\SHEETS\PLAN\SIGNING & MARKING\023201-PS_STH 13.DWG

LAYOUT NAME - SHEET - (51)

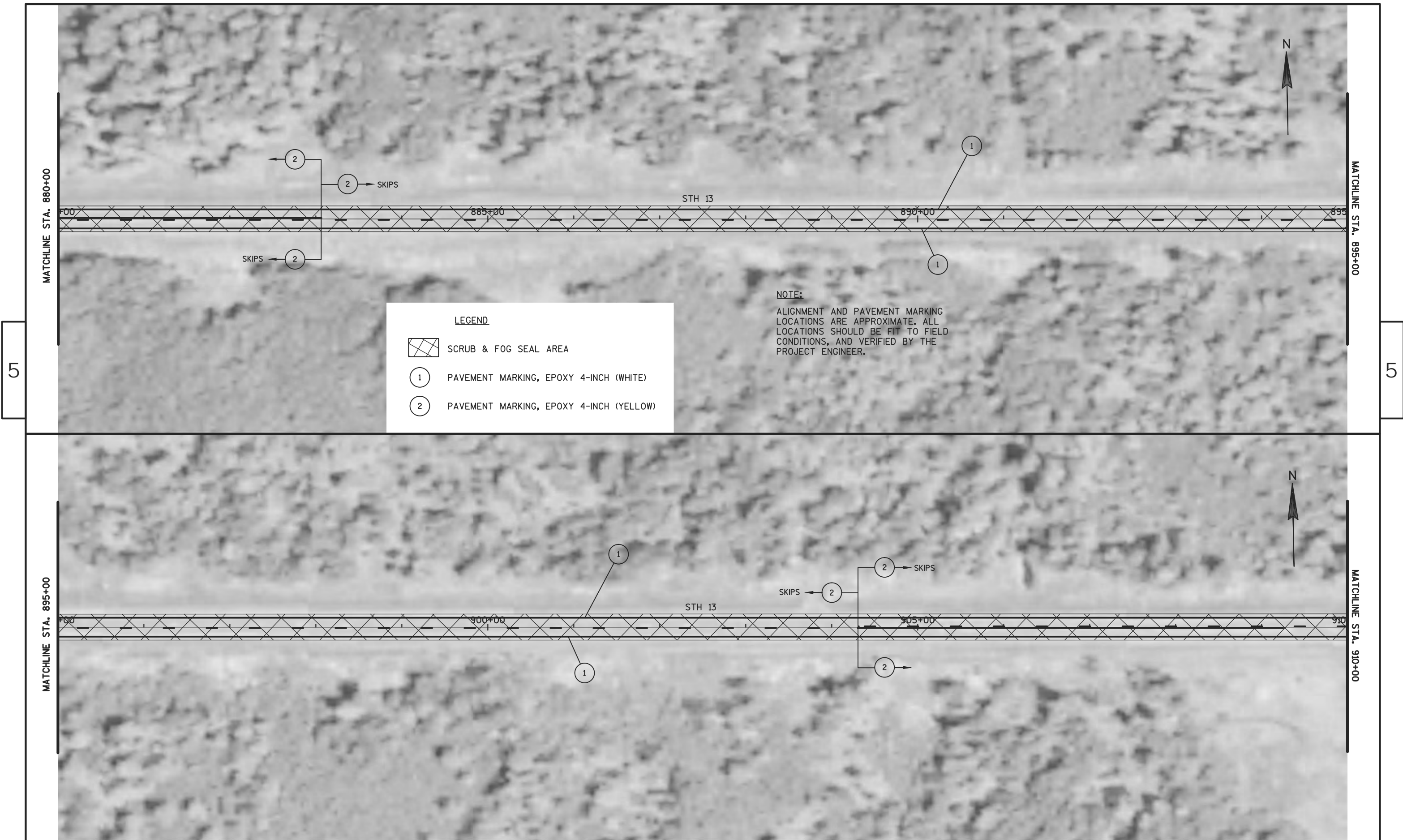
PLOT DATE : 9/28/2016 3:02 PM

PLOT BY : JENNIFER LAUER

PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADDs SHEET 44



PROJECT NO: 8510-01-60

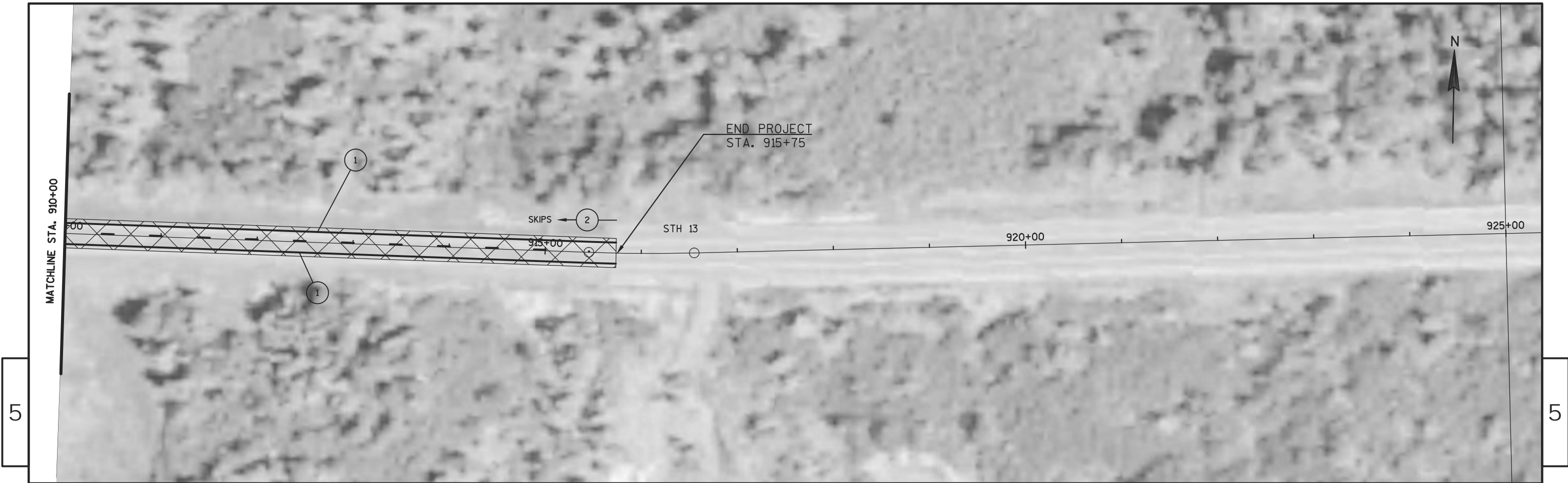
HWY: STH 13

COUNTY: DOUGLAS




PLAN DETAILS

SHEET

E



LEGEND

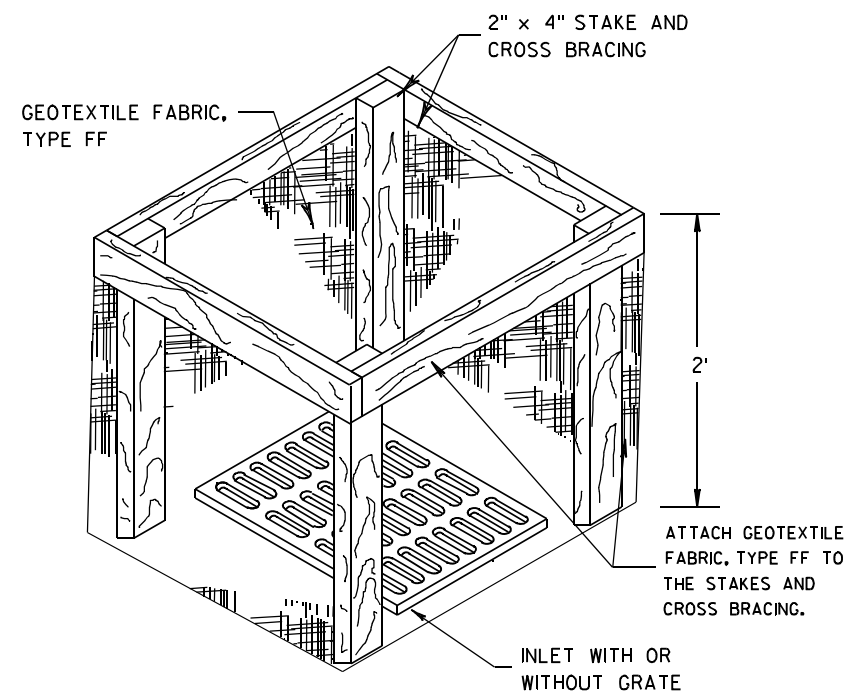
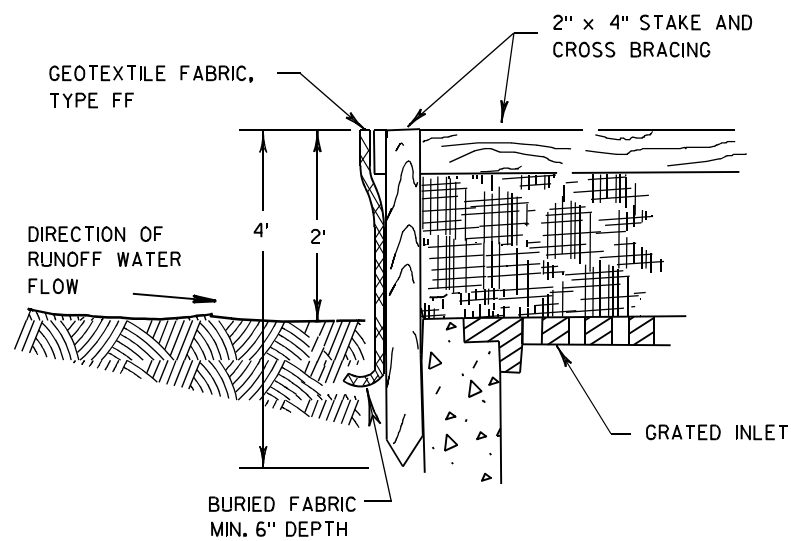
-  SCRUB & FOG SEAL AREA
-  PAVEMENT MARKING, EPOXY 4-INCH (WHITE)
-  PAVEMENT MARKING, EPOXY 4-INCH (YELLOW)

NOTE:

ALIGNMENT AND PAVEMENT MARKING LOCATIONS ARE APPROXIMATE. ALL LOCATIONS SHOULD BE FIT TO FIELD CONDITIONS, AND VERIFIED BY THE PROJECT ENGINEER.

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C04-03	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-03	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C06-08	SIGNING & MARKING FOR TWO LANE BRIDGES
15C07-13B	PAVEMENT MARKING WORDS
15C07-13C	PAVEMENT MARKING ARROWS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C08-16C	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16D	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16E	PAVEMENT MARKING (LEFT TURN LANE)
15C08-16F	PAVEMENT MARKING (ISLANDS)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C18-03	MEDIAN ISLAND MARKING
15C19-04A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-02	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C34-01	STANDARD APPLICATION FOR TEMPORARY RAISED PAVEMENT MARKER, TYPE 2
15D28-03	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY



INLET PROTECTION, TYPE A

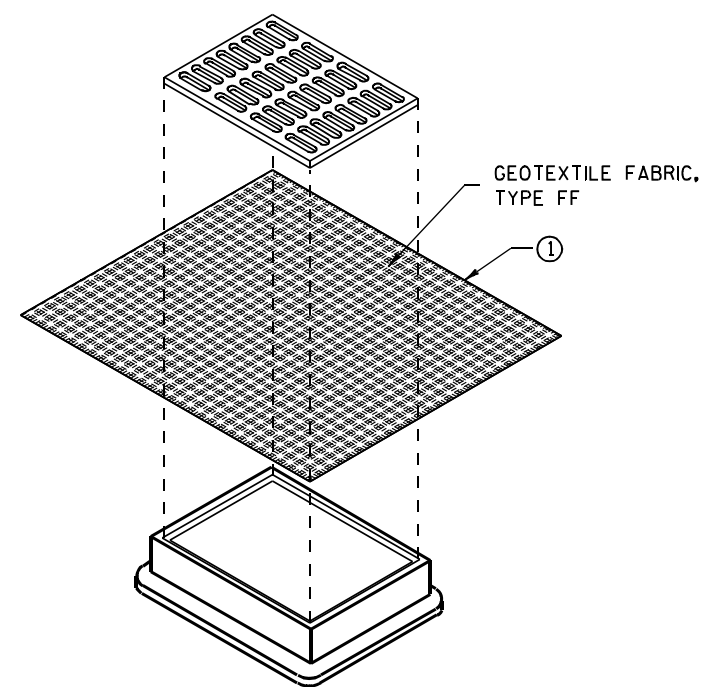
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

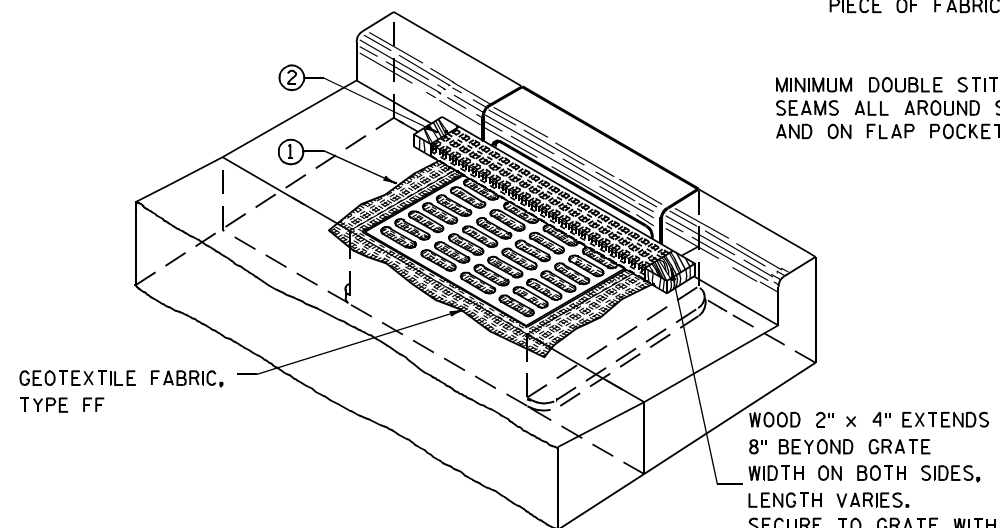
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

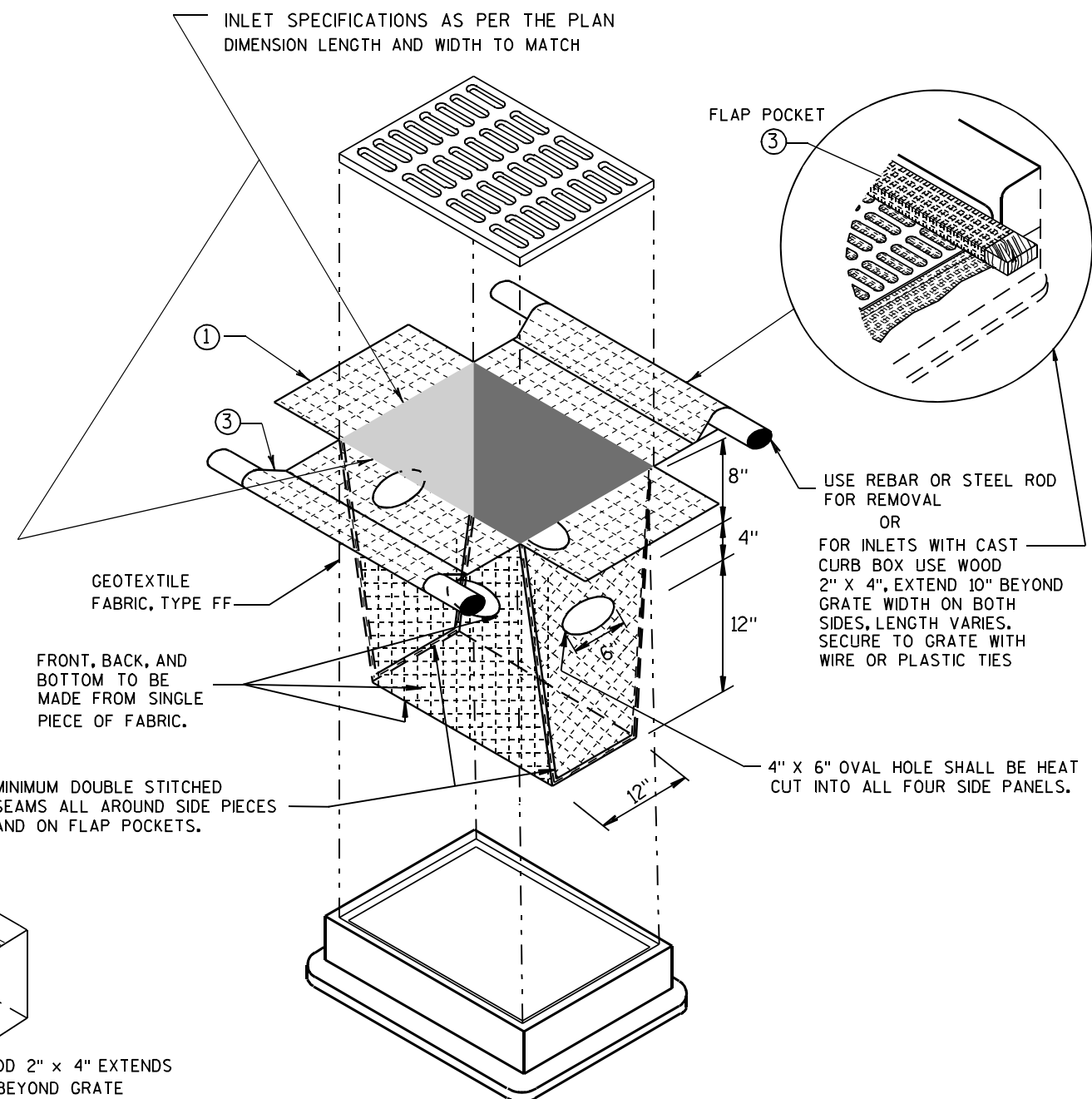
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



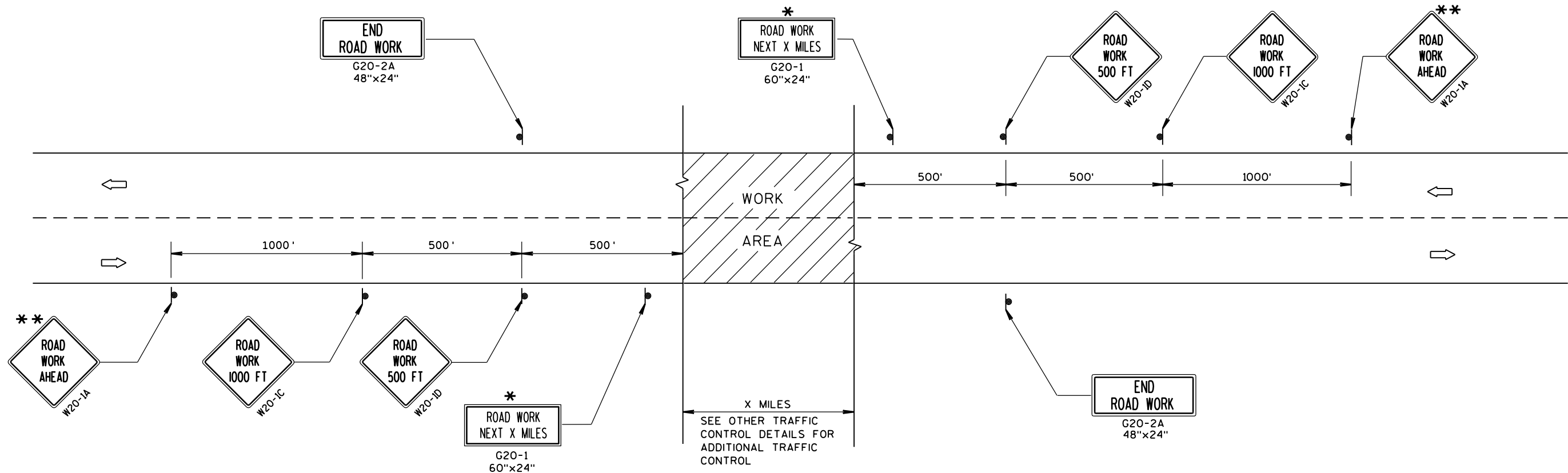
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Cannestra
DATE
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

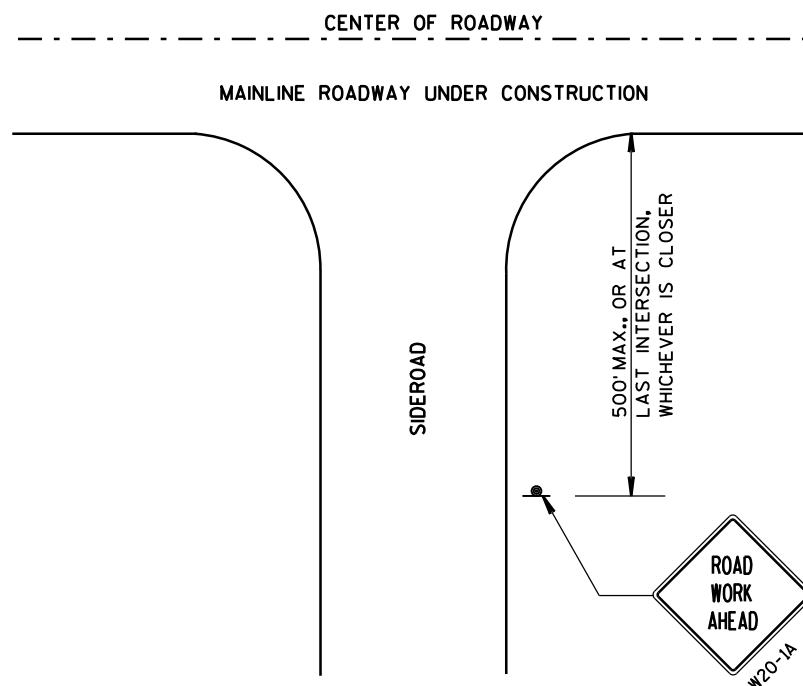
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

** PLACE ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



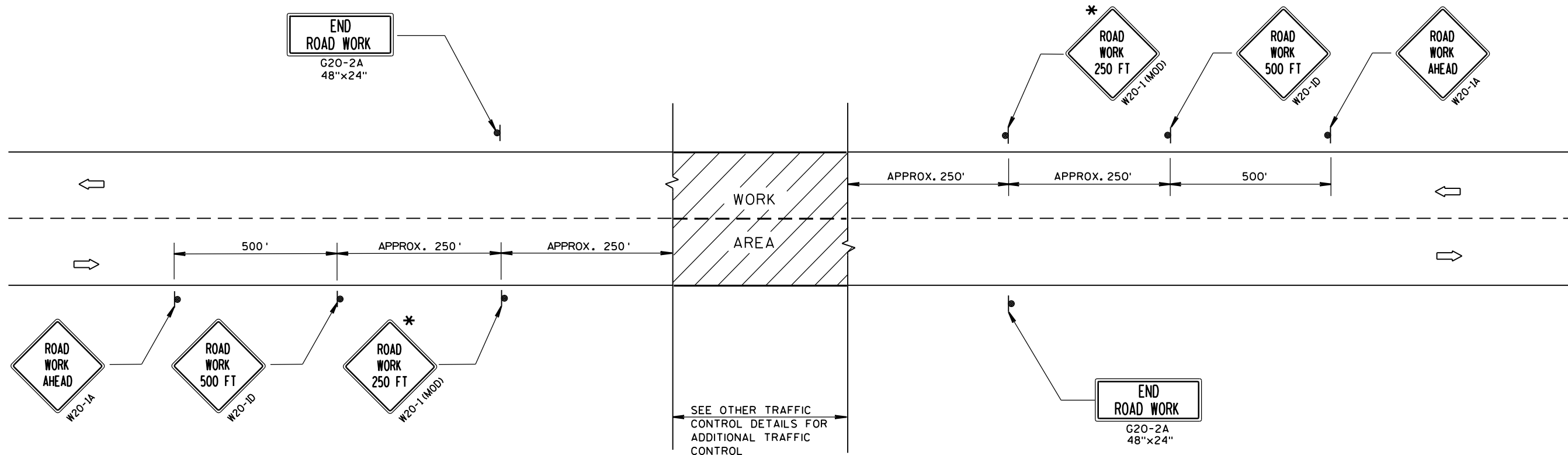
LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 45 M.P.H.
OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2015 /S/ Peter Amokobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

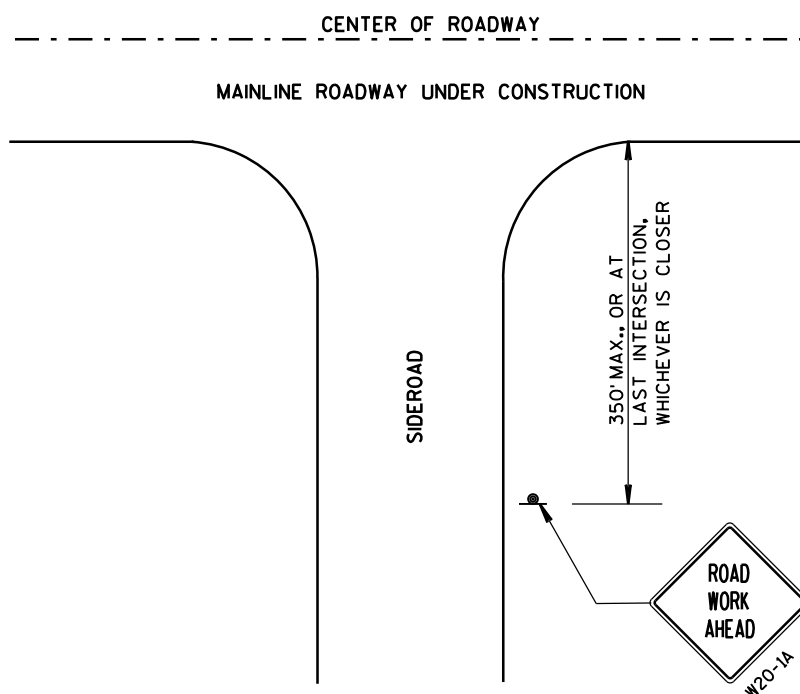
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"x36" SIGNS MAY BE USED INSTEAD OF 48"x48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



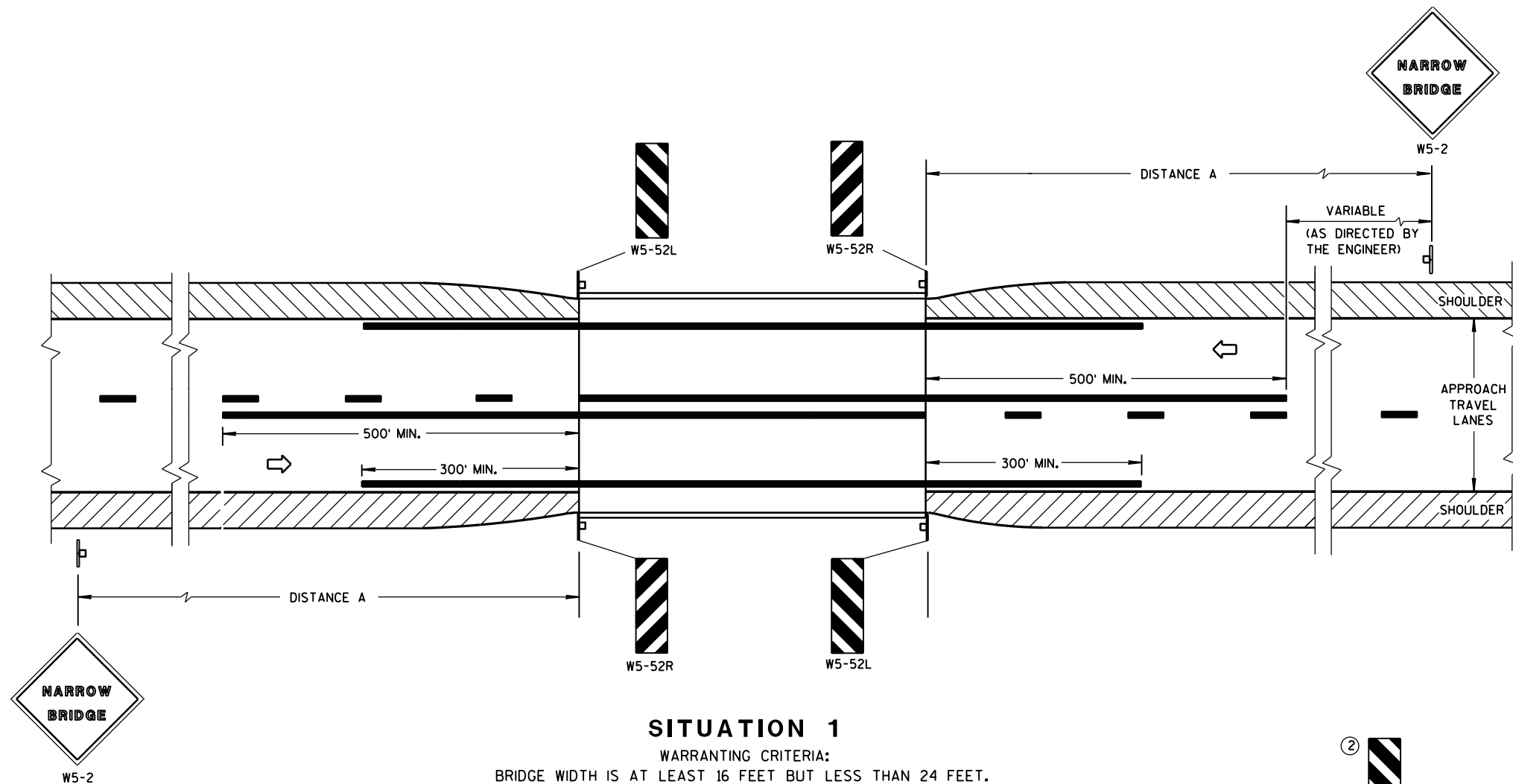
LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 40 M.P.H.
OR LESS TWO-WAY UNDIVIDED
ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2015 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



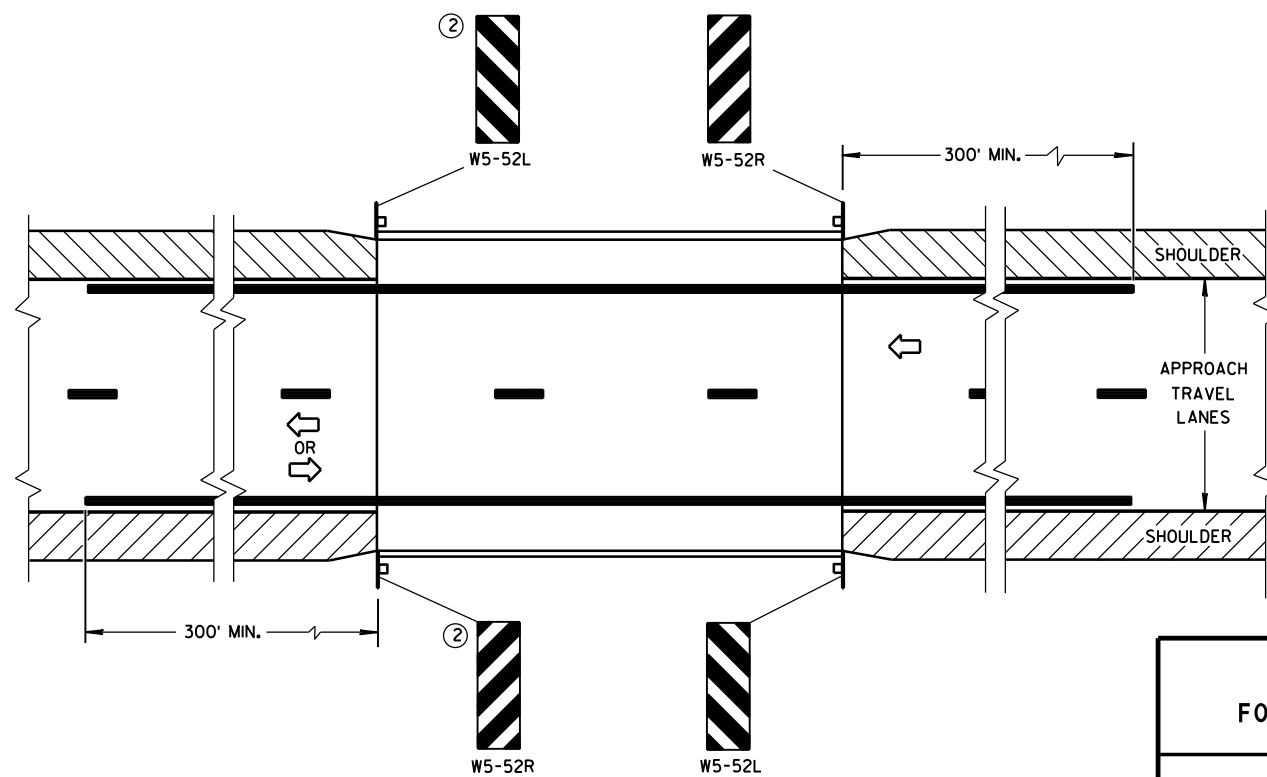
DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	750'

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

- ① LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.
- ② OMIT ON ONE-WAY TRAVELLED WAYS.
- ③ EDGE OF W5-52 SIGN SHALL BE PLACED IN LINE WITH FACE OF CURB OR PARAPET.

SIGNING & MARKING
FOR TWO LANE BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

4-18-16

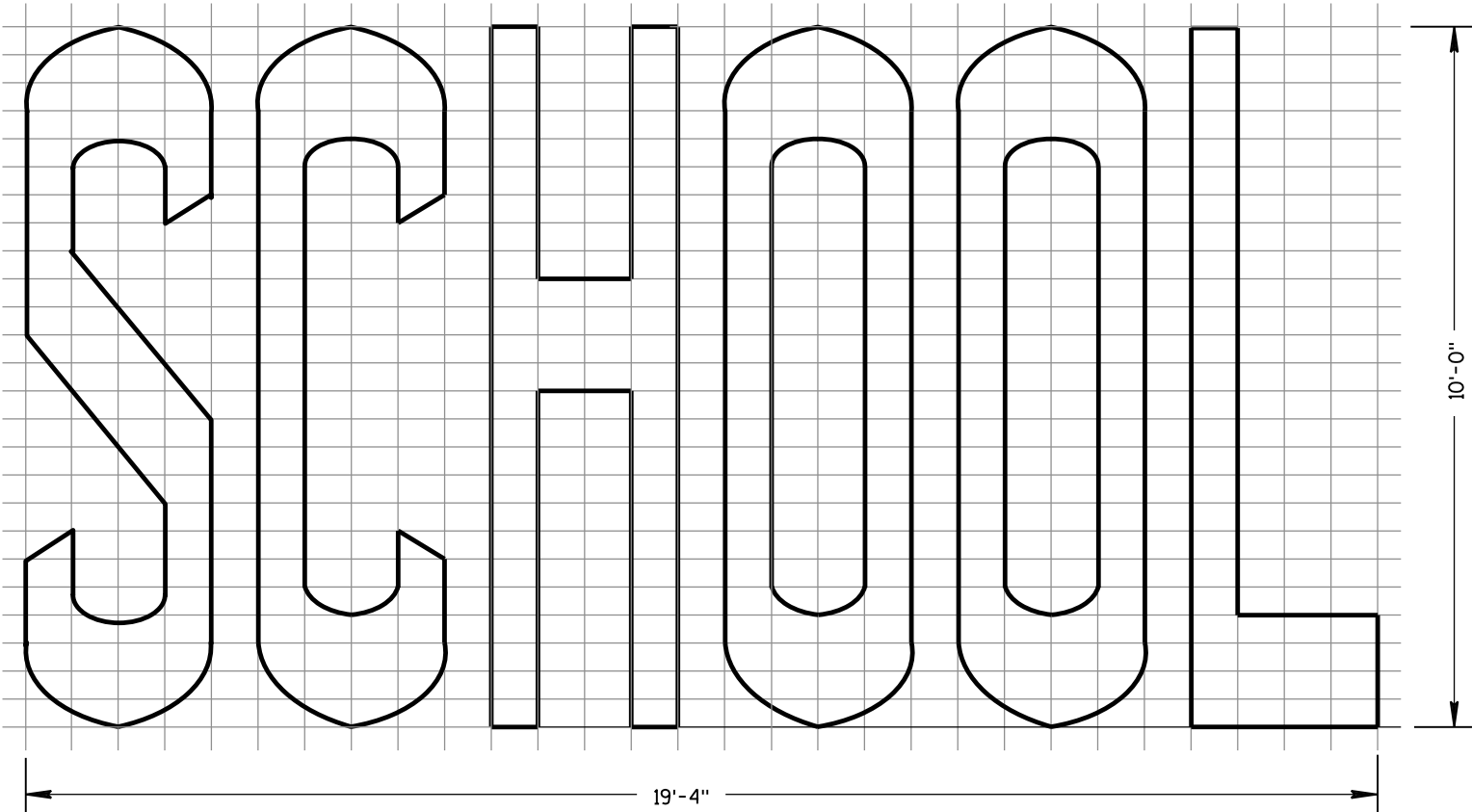
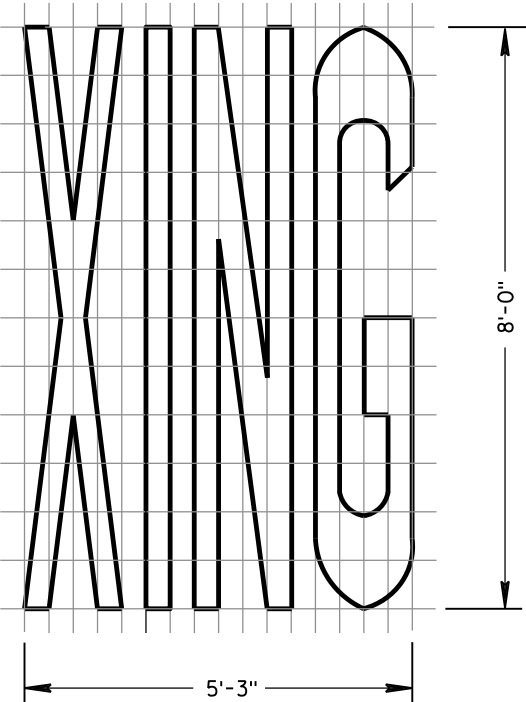
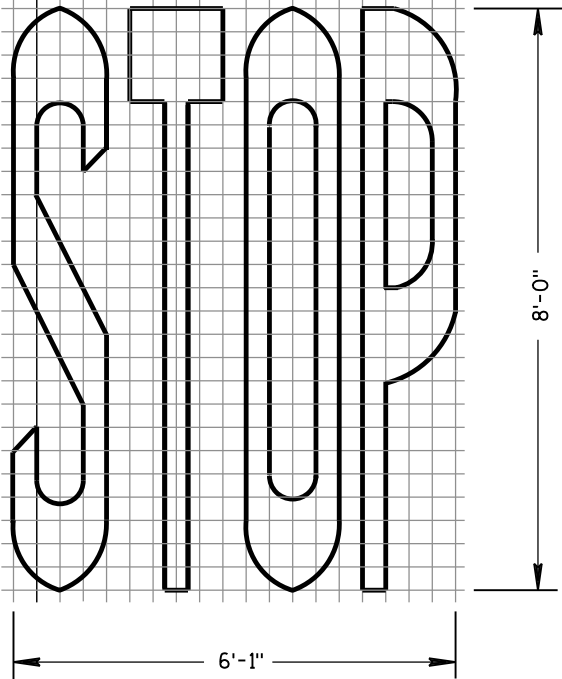
DATE

FHWA

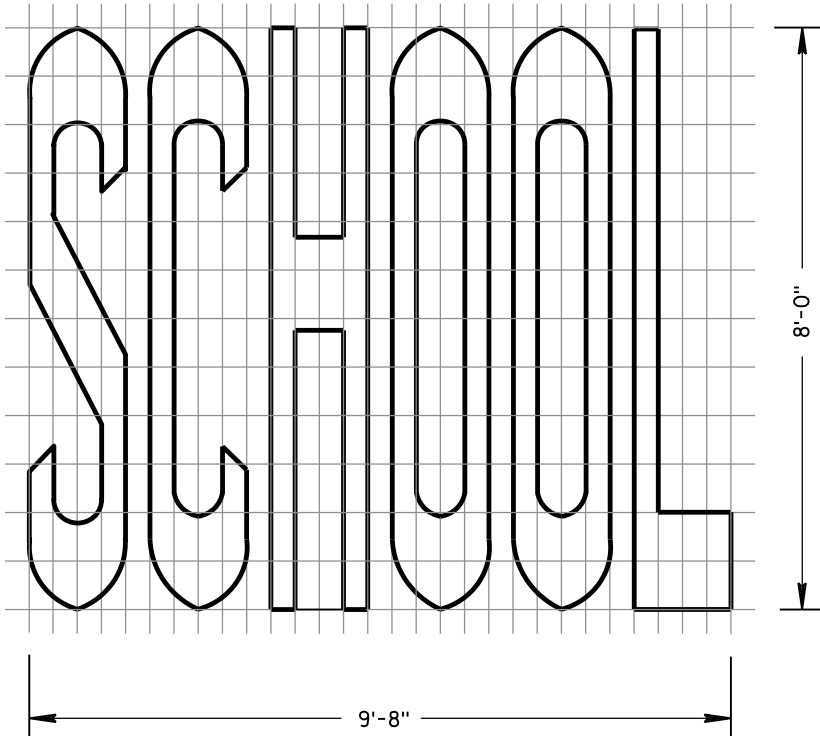
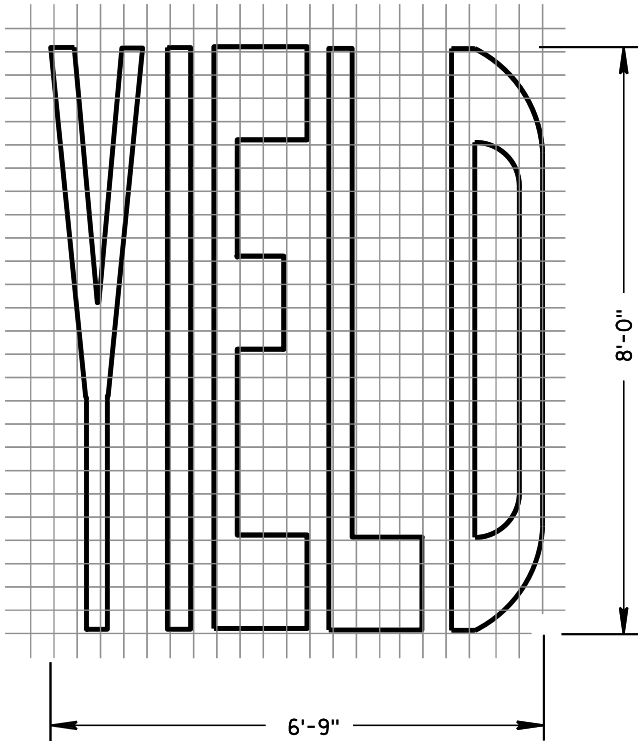
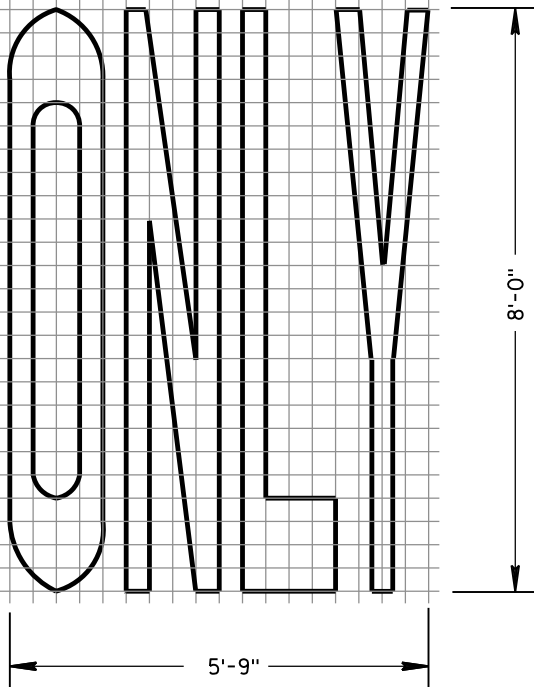
/S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER

GENERAL NOTES

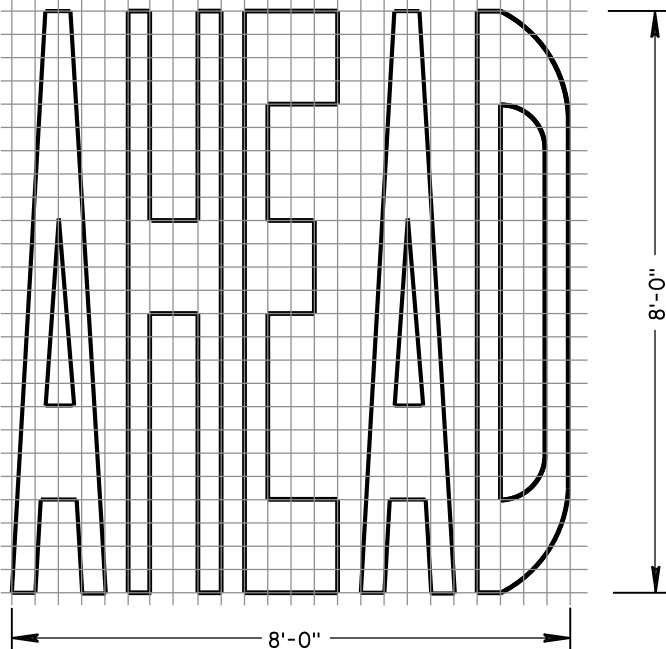
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



TWO-LANE



SINGLE-LANE



PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

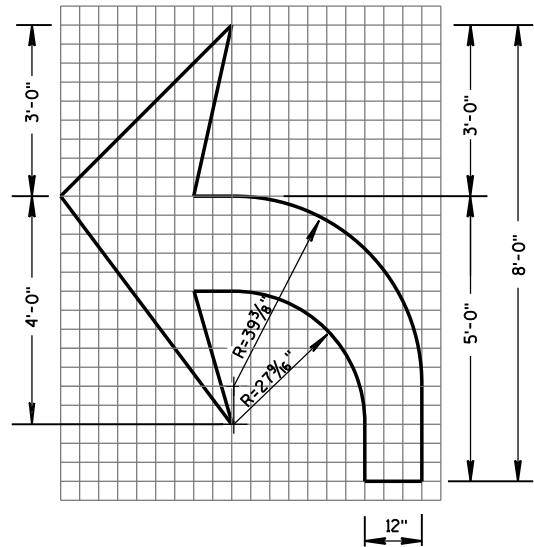
APPROVED

4-18-16

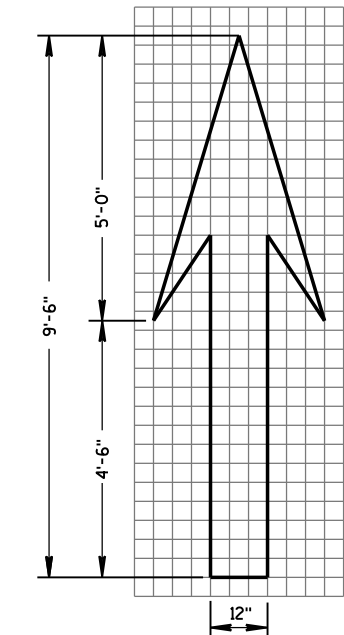
DATE

FHWA

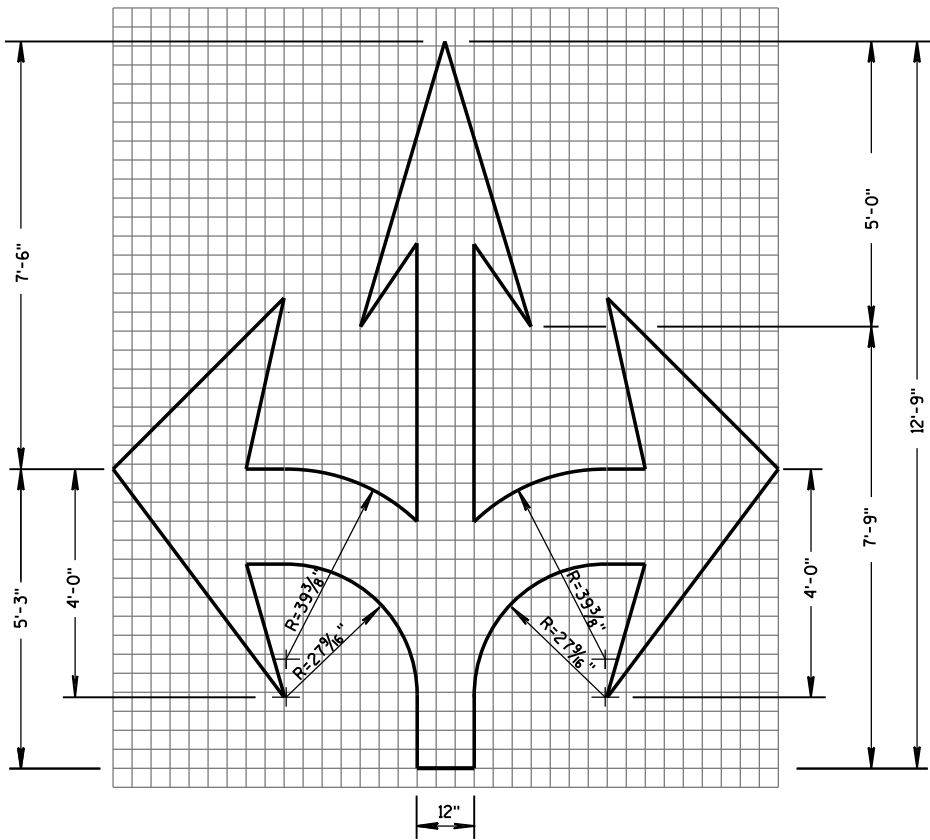
/S/ Matthew R. Rauch
STATE SIGNING AND MARKING ENGINEER



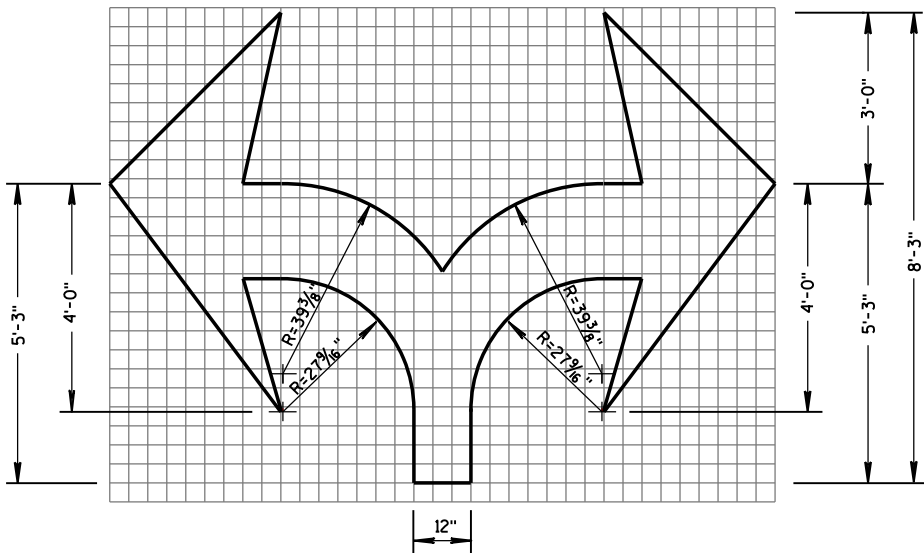
TYPE 2



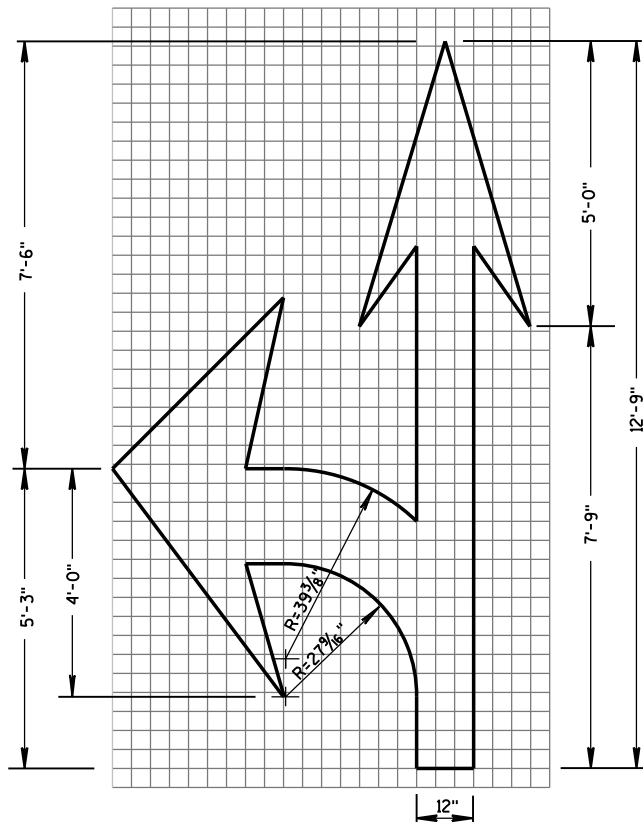
TYPE 1



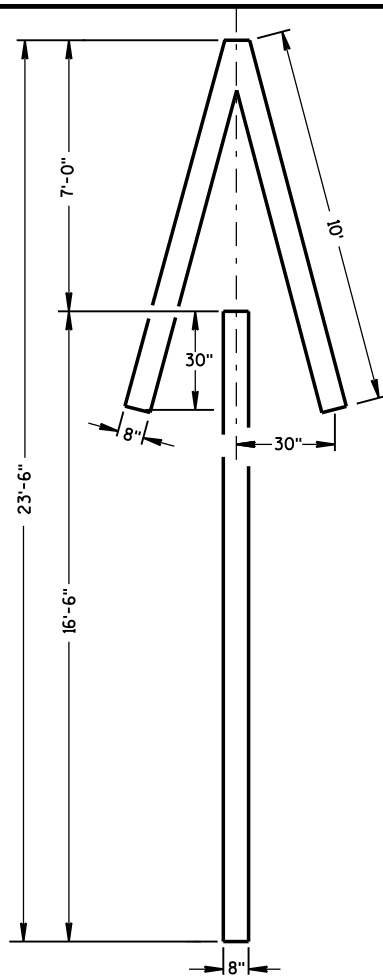
TYPE 6



TYPE 7



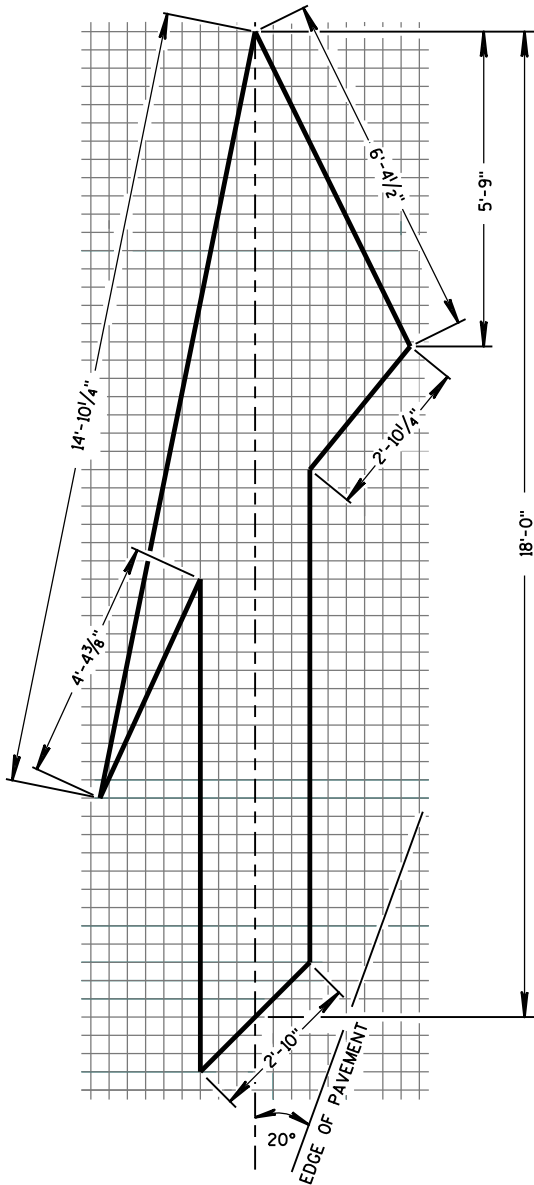
TYPE 3



TYPE 4

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

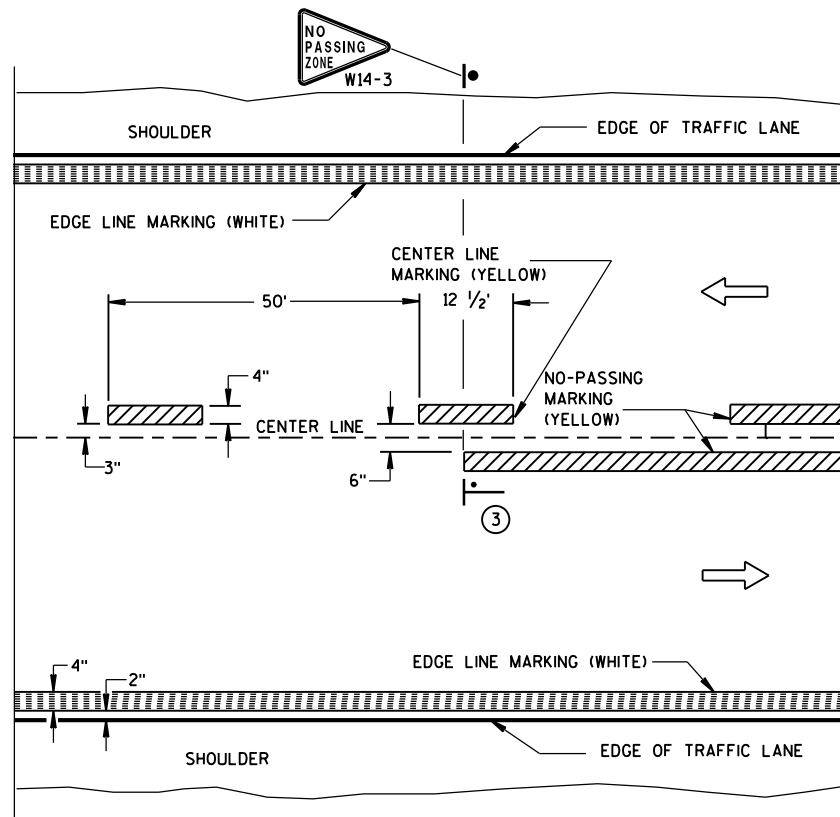


TYPE 5 LANE DROP ARROW

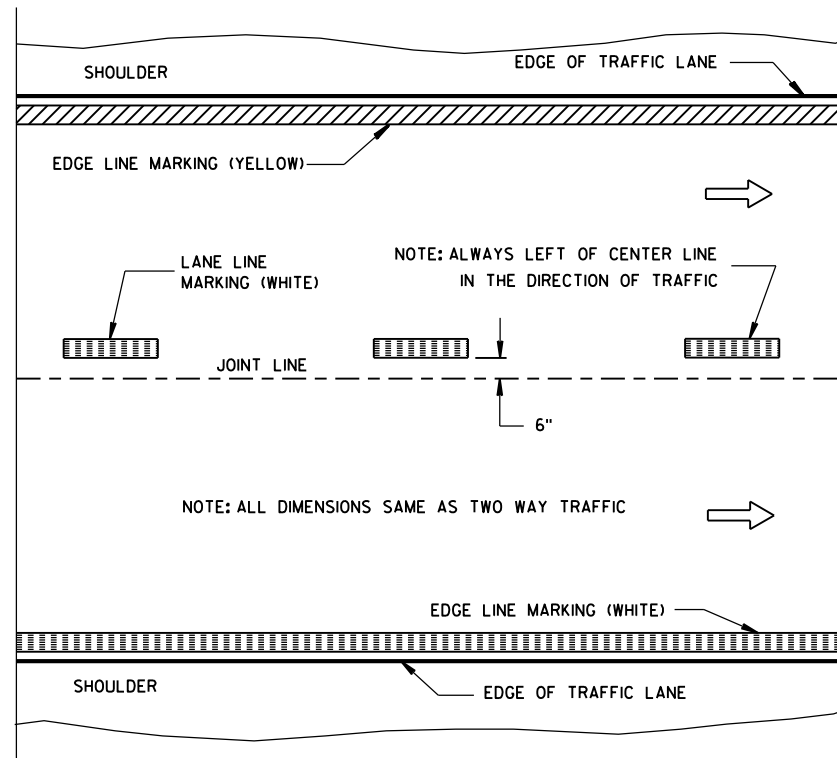
PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-18-16 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA

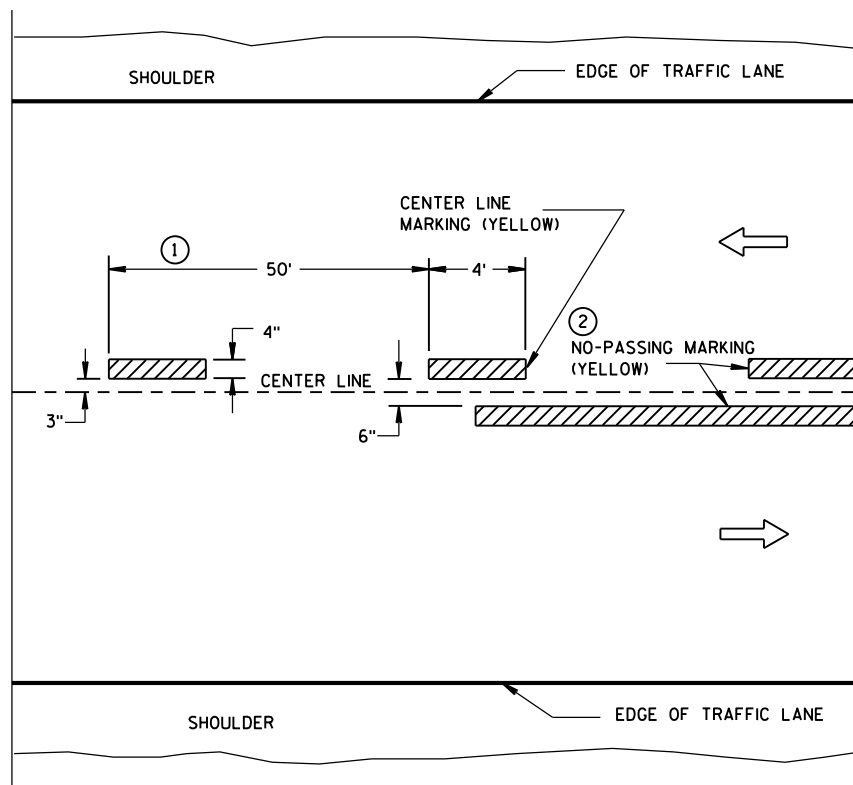


TWO WAY TRAFFIC

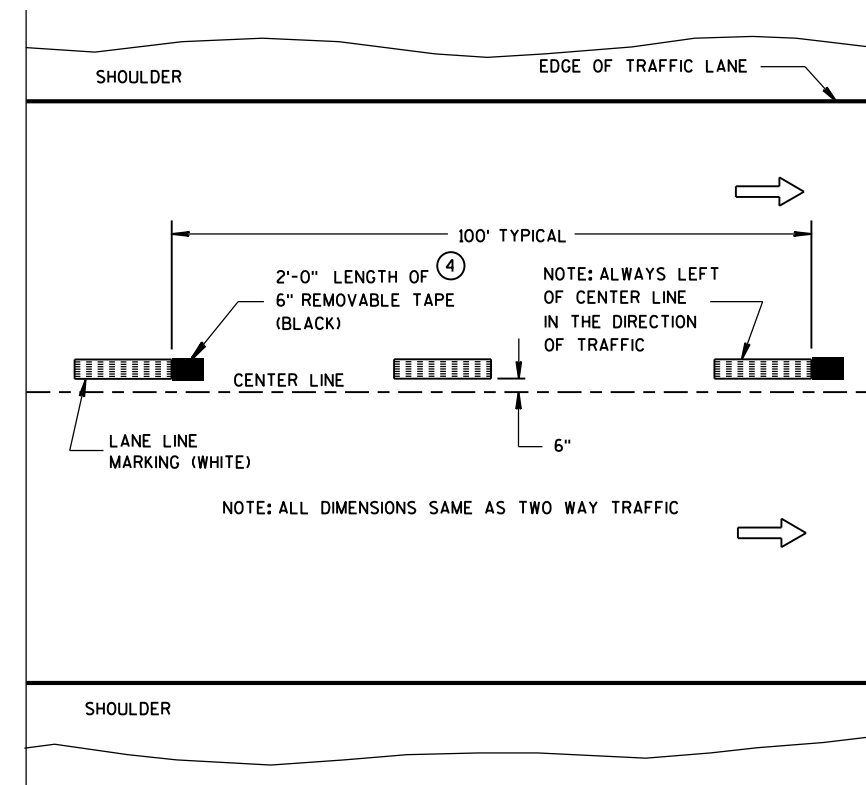


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

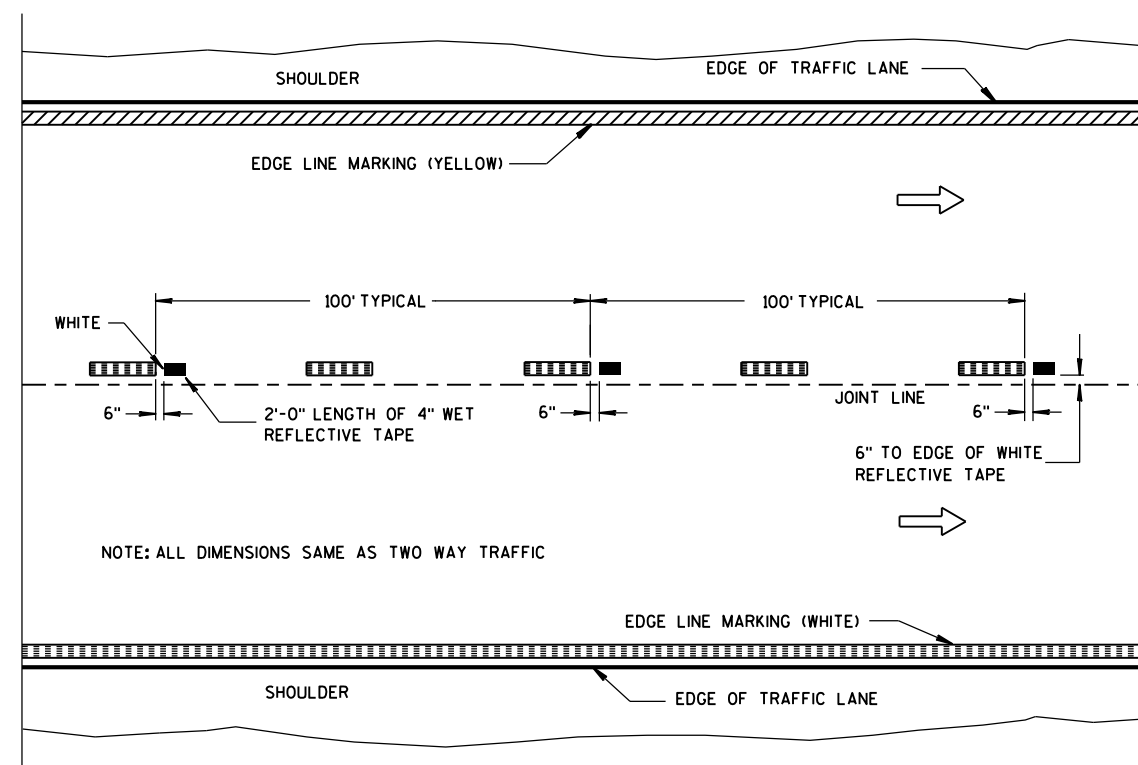
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- ② NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- ③ NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- ④ CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

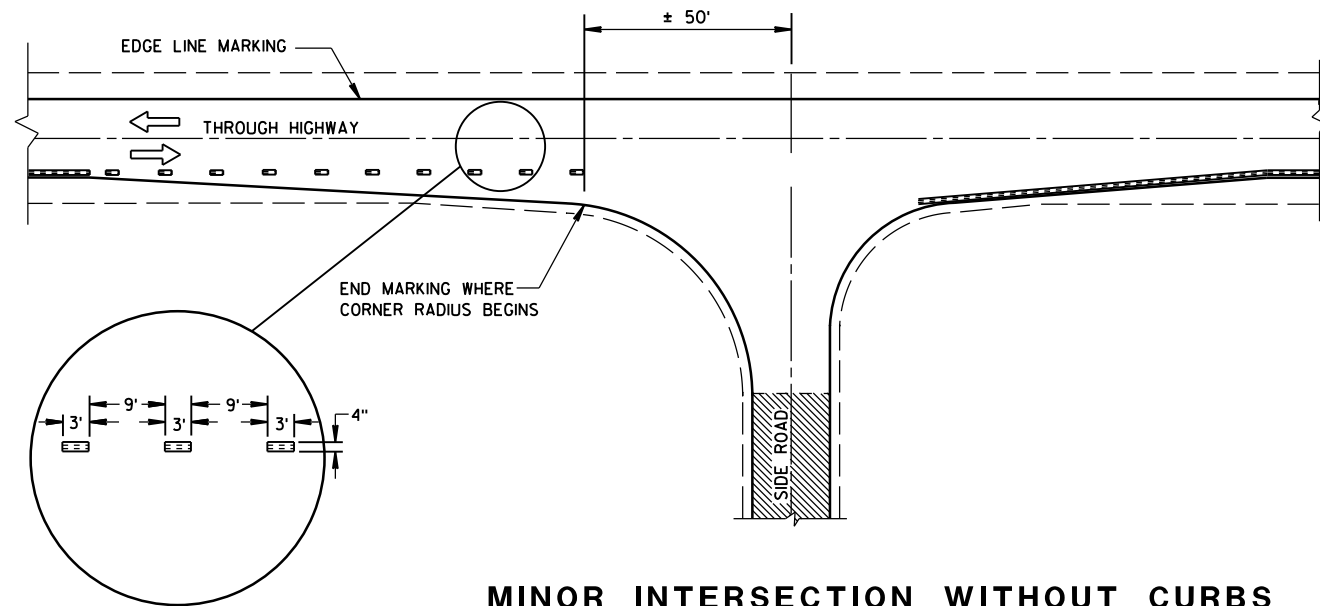
- "T" MARKING
- POST MOUNTED SIGN

PAVEMENT MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013
DATE
FHWA

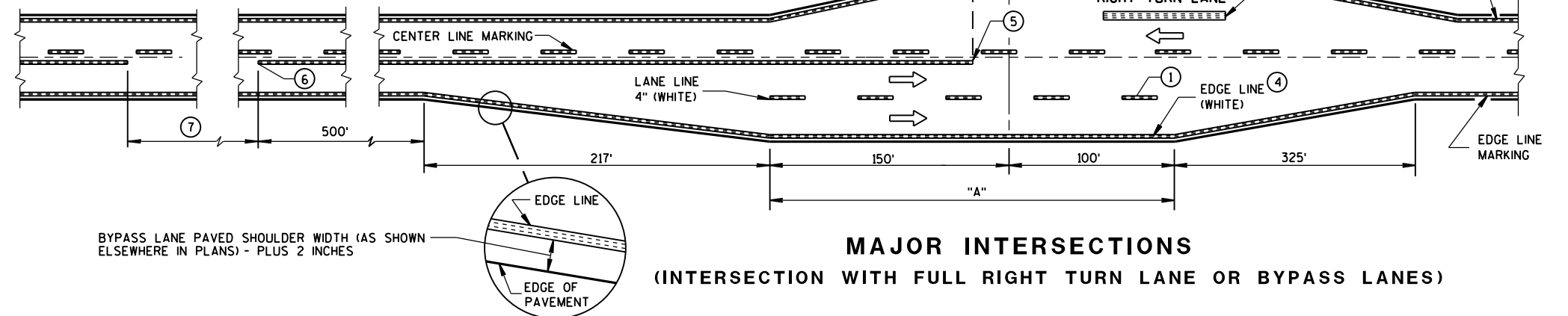
/S/ Travis Feltes
STATE TRAFFIC ENGINEER



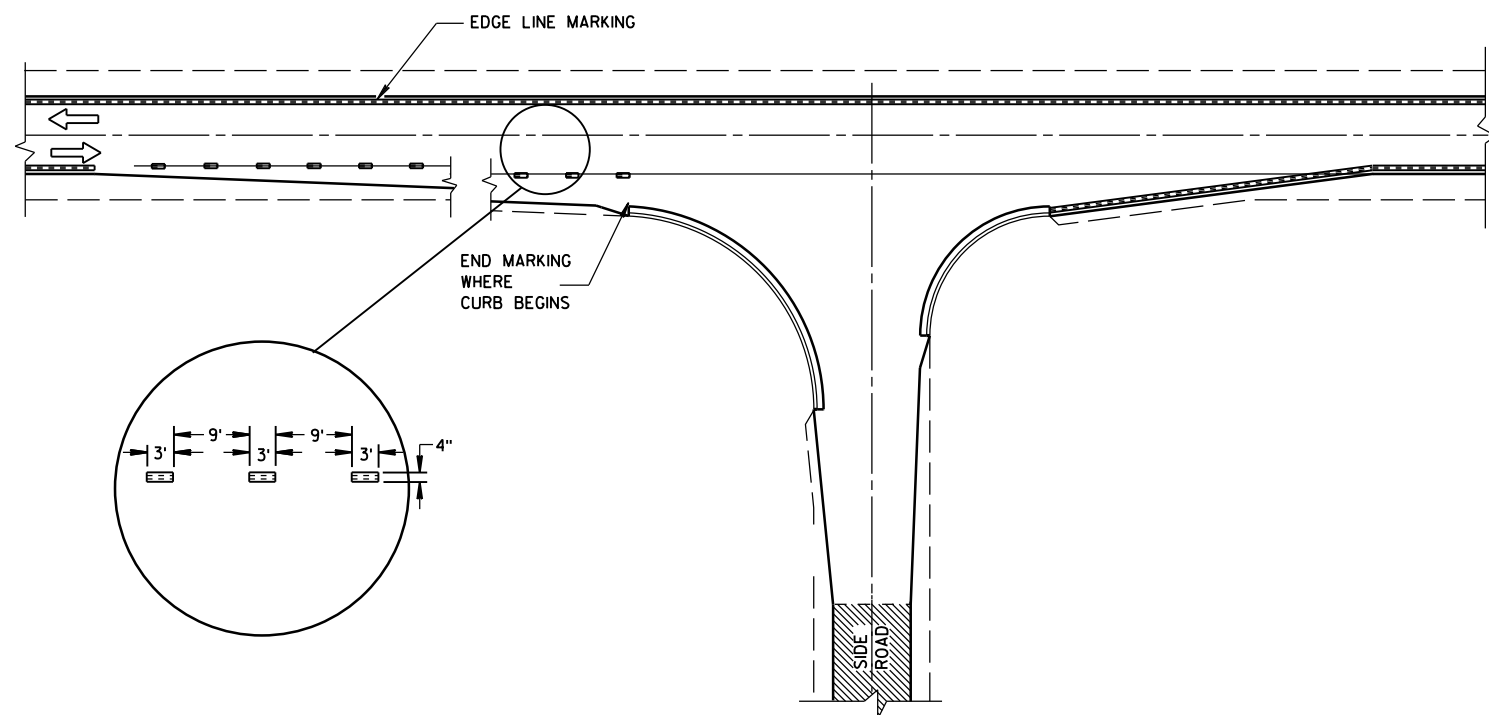
MINOR INTERSECTION WITHOUT CURBS

⑦

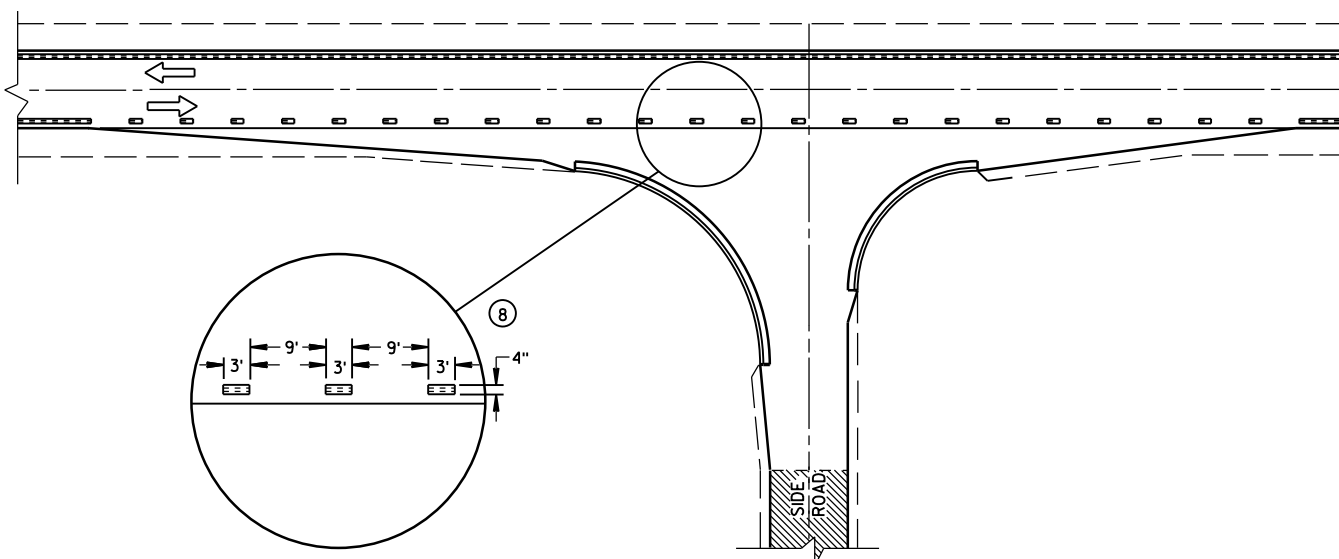
POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)



MINOR INTERSECTION WITH CURBS
(TYPICAL MARKING)



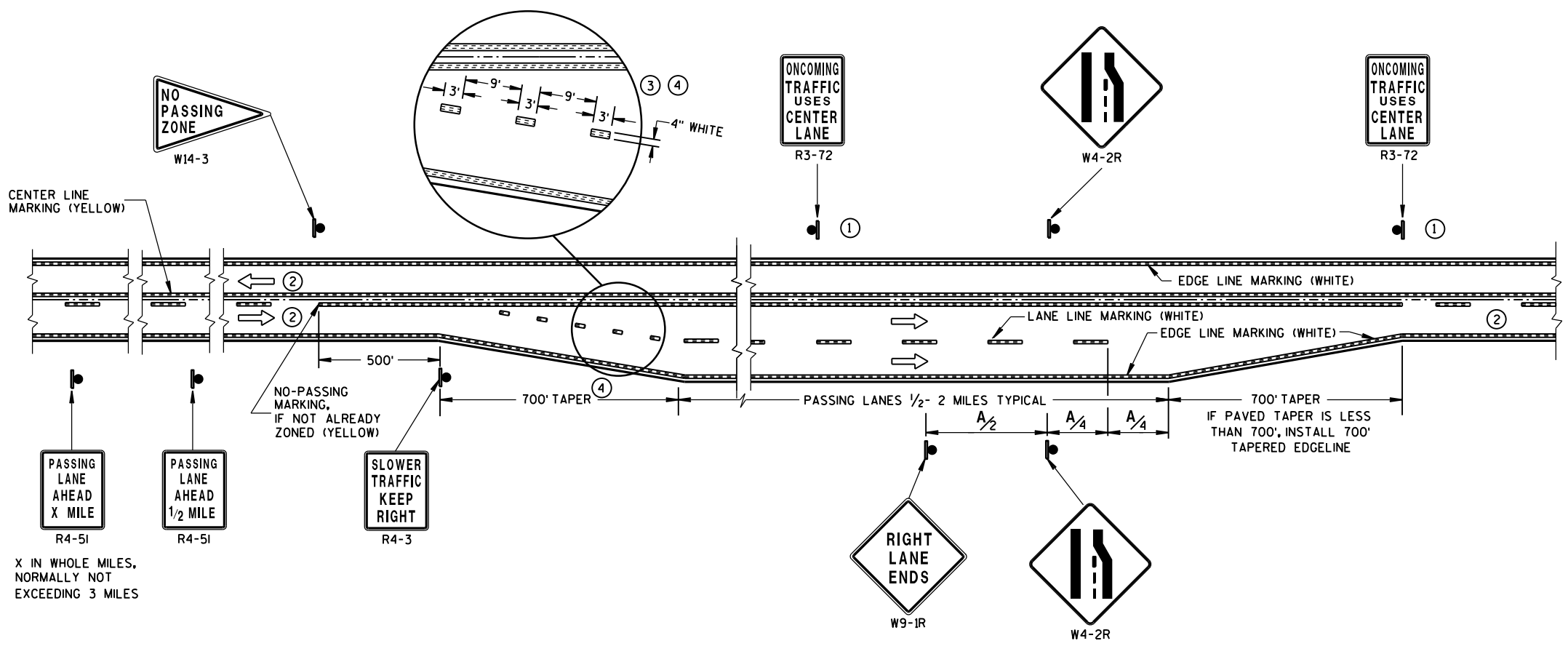
MINOR INTERSECTION WITH CURBS
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)

GENERAL NOTES

- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
 - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
 - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
 - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.
 - ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
 - ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
 - ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
 - ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



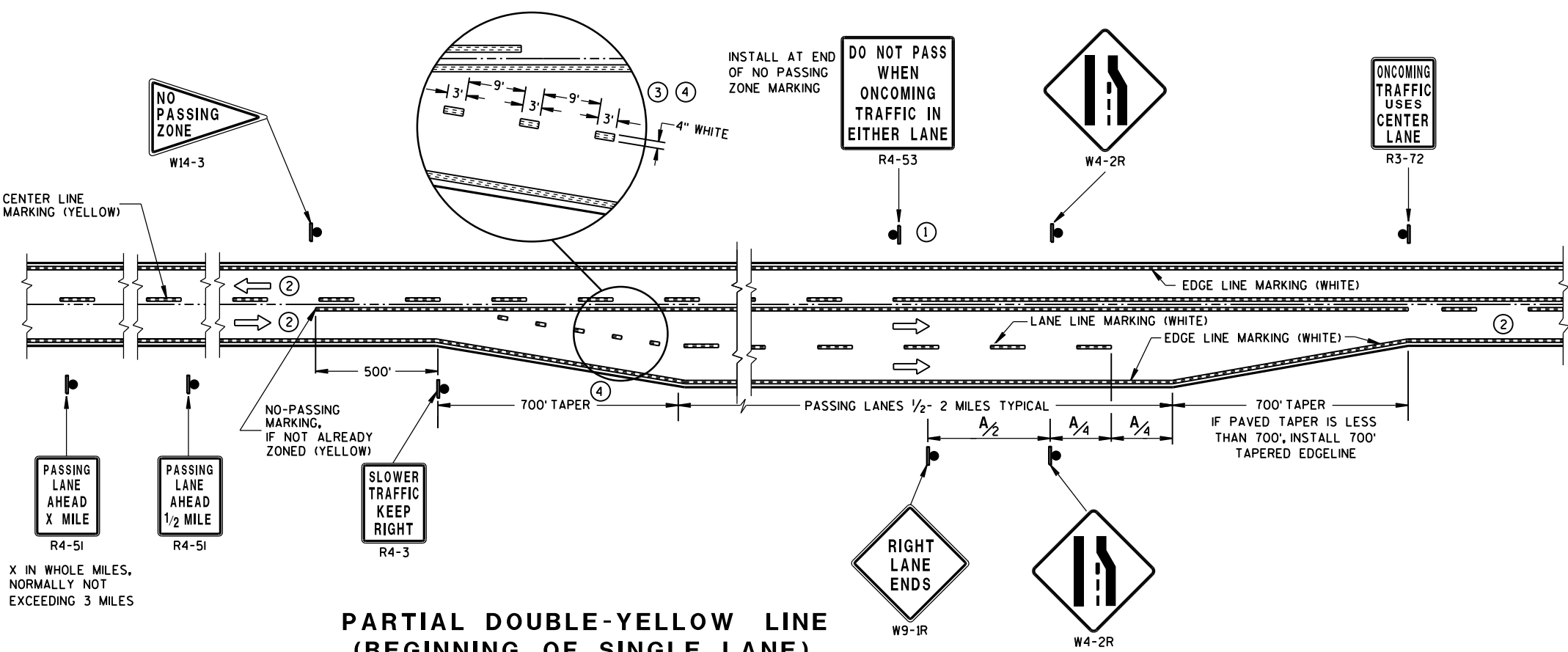
**SOLID DOUBLE-YELLOW LINE
(THROUGHOUT ENTIRE PASSING/CLIMBING LANE)**

GENERAL NOTES

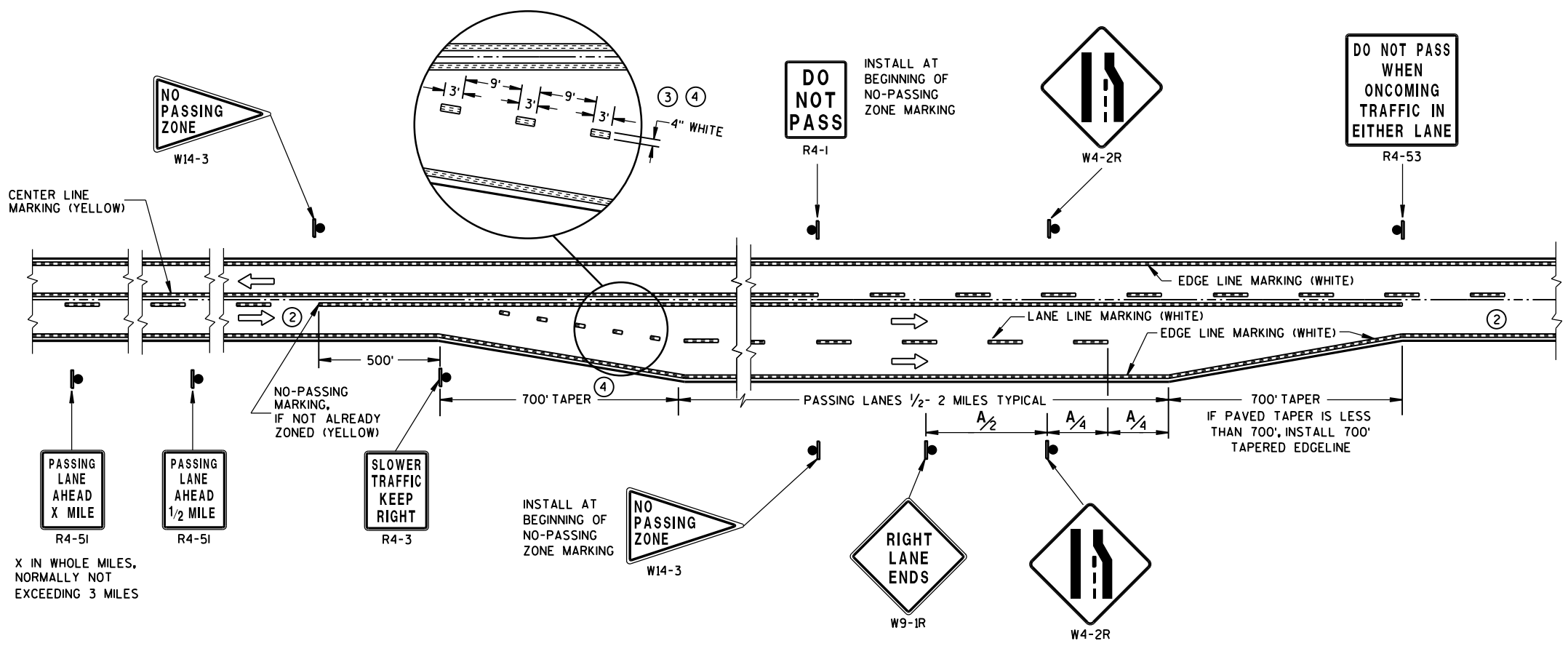
- ① SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (➡) SHOWS DIRECTION OF TRAVEL

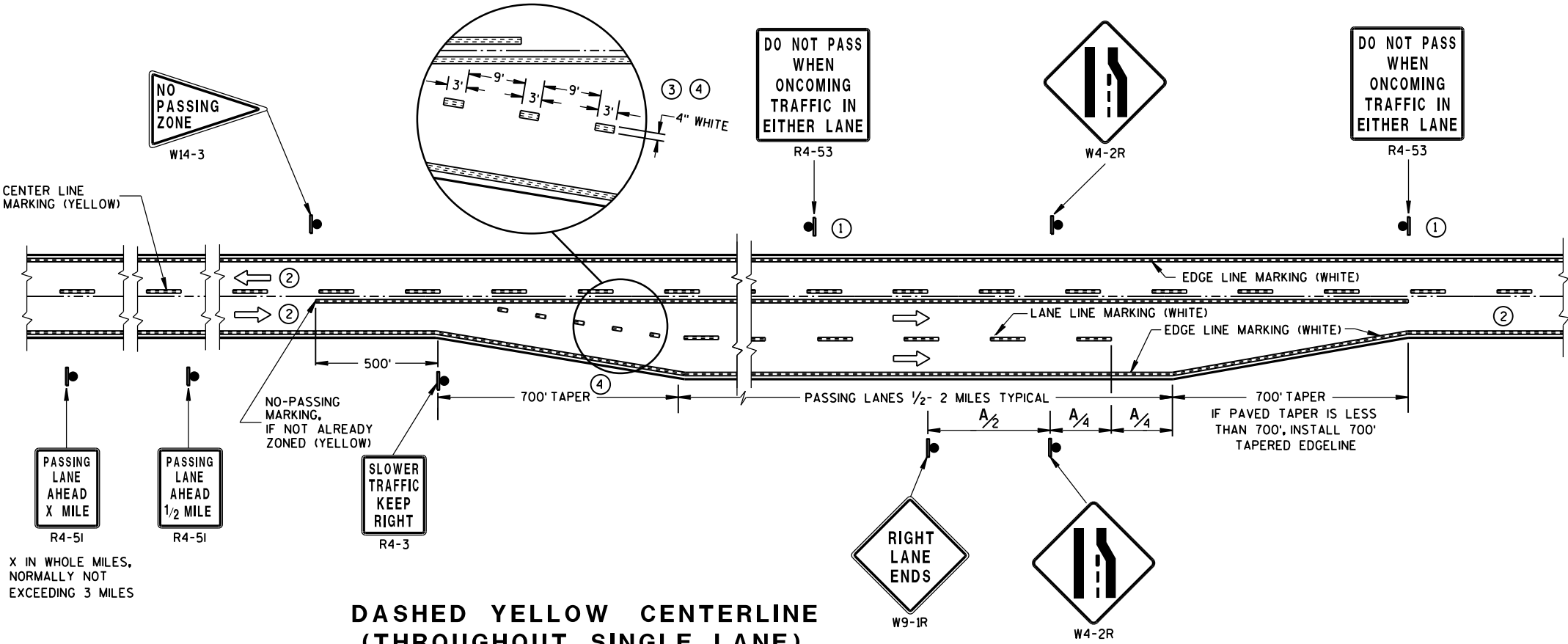
DISTANCE TABLE	
POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950



**PARTIAL DOUBLE-YELLOW LINE
(BEGINNING OF SINGLE LANE)**



**SOLID DOUBLE-YELLOW LINE
(END OF SINGLE LANE)**



**DASHED YELLOW CENTERLINE
(THROUGHOUT SINGLE LANE)**

GENERAL NOTES

- ① SIGN SHALL BE REPEATED AT 1/2 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

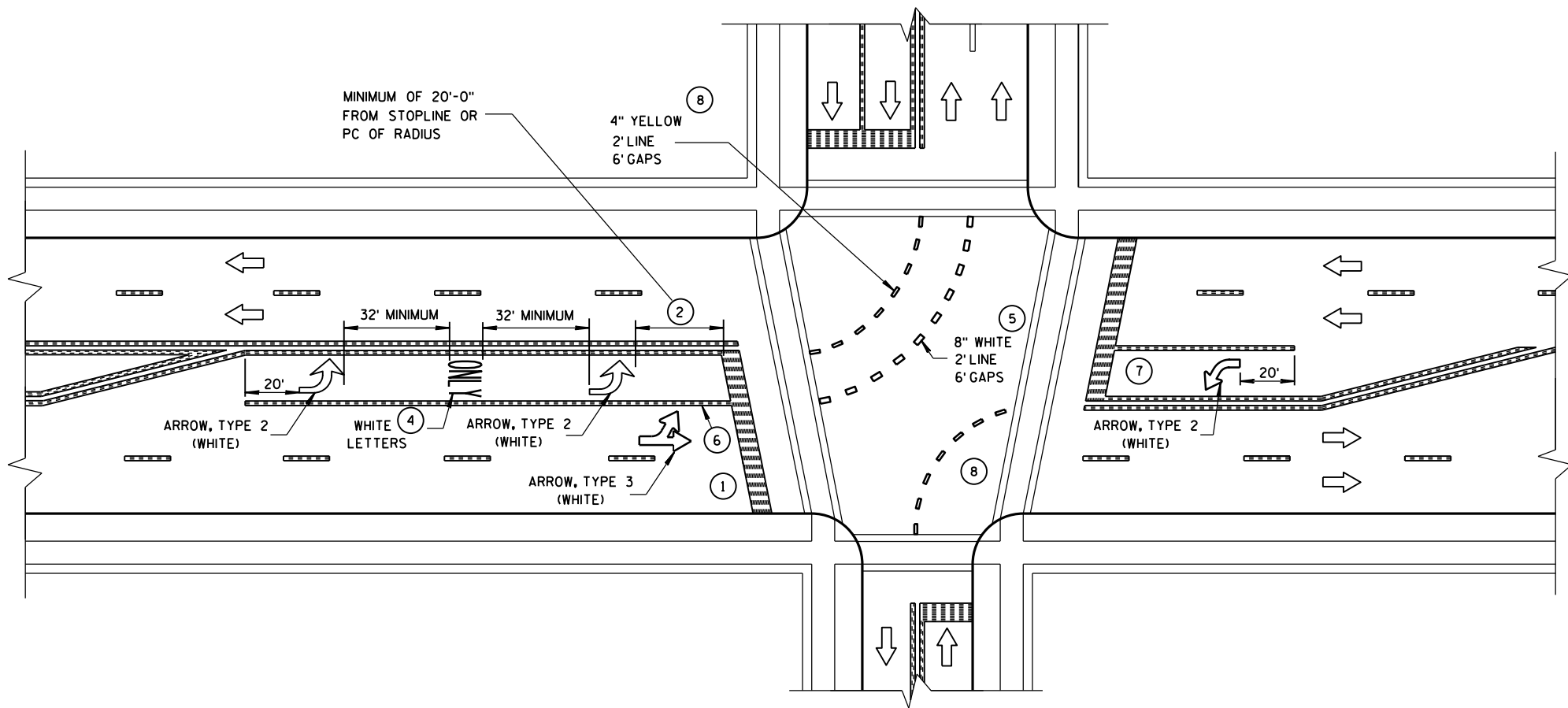
ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950

**PAVEMENT MARKING & SIGNING
(CLIMBING LANE &
PASSING LANE)**

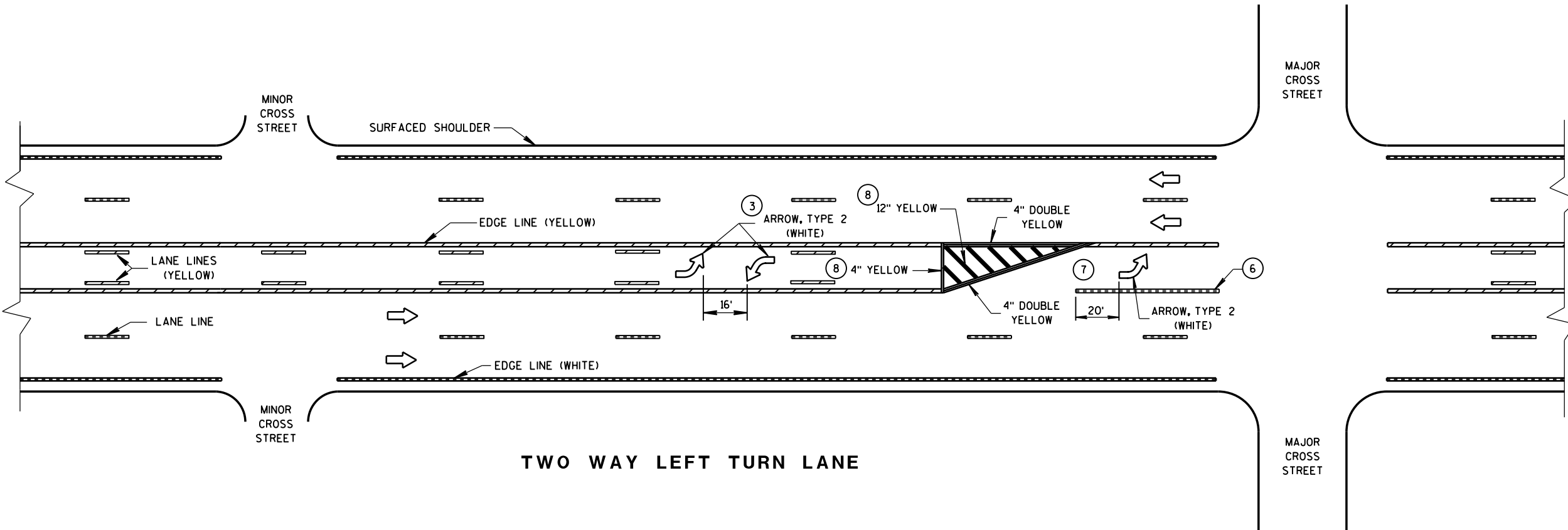
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



GENERAL NOTES

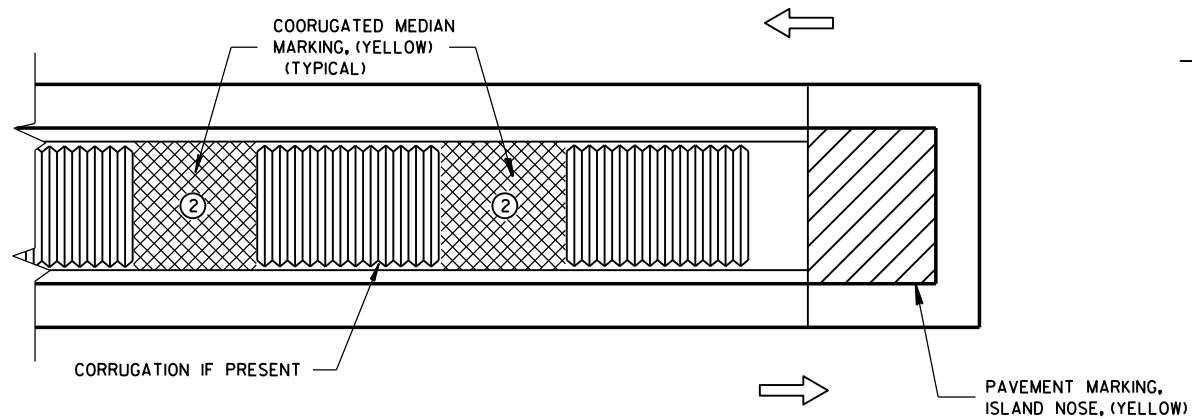
- 1 STOP BAR IS REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.
- 2 DISTANCE MAY BE ADJUSTED TO ACCOMODATE SHORT LEFT TURN LANES. AS APPROVED BY THE ENGINEER.
- 3 A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- 4 ADD EXTRA SETS OF ONE ARROW AND ONE ONLY PER 160 FEET OR WHEN ON A CURVE.
- 5 8" WHITE WITH 2' LINE 6' GAPS FOR DUAL TURN LANE.
- 6 8" WHITE
- 7 ADD SECOND ARROW WHEN TURN BAY IS GREATER THAN OR EQUAL TO 108 FEET.
- 8 REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.

NOTE:
ARROW SYMBOL (➡)
SHOWS DIRECTION OF TRAVEL

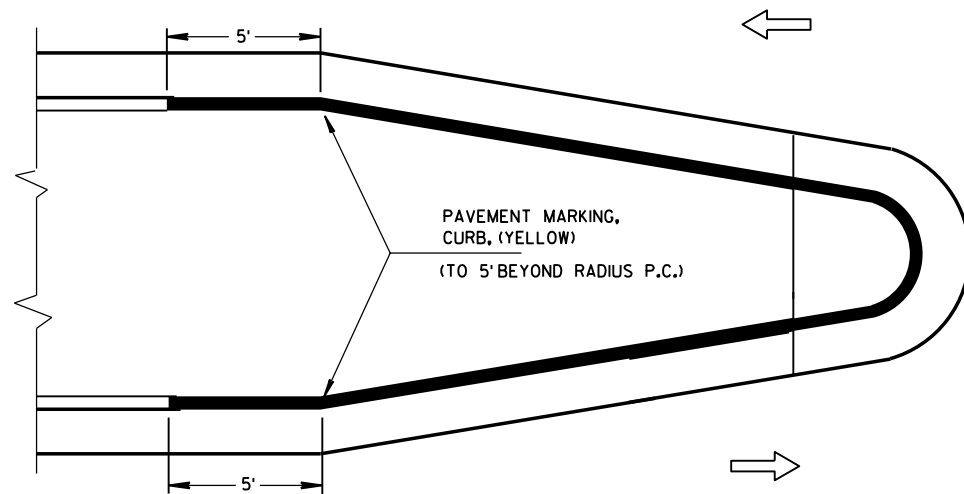


PAVEMENT MARKING
(LEFT TURN LANE)

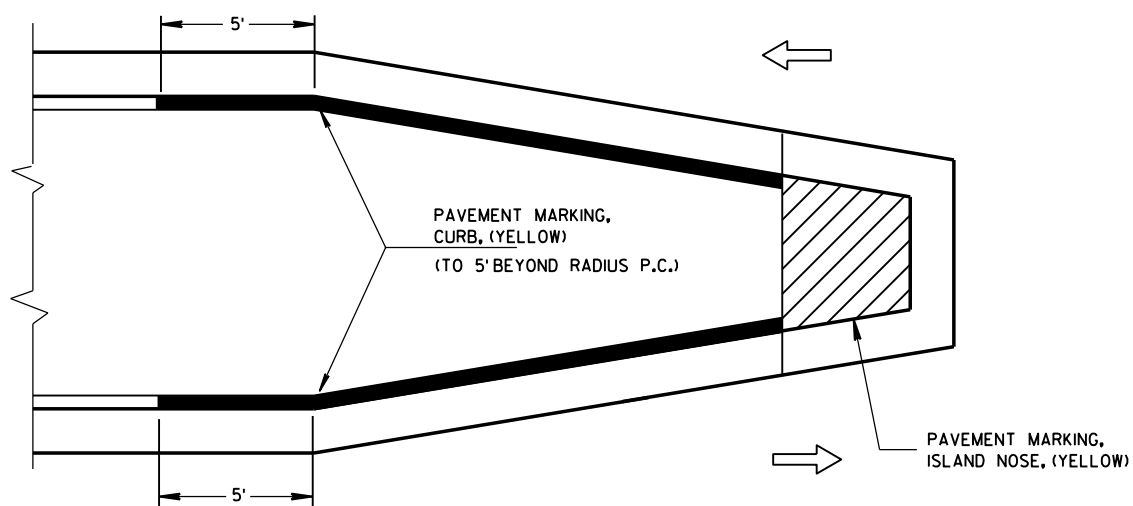
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MEDIAN ISLAND WITH SQUARE BLUNT NOSE

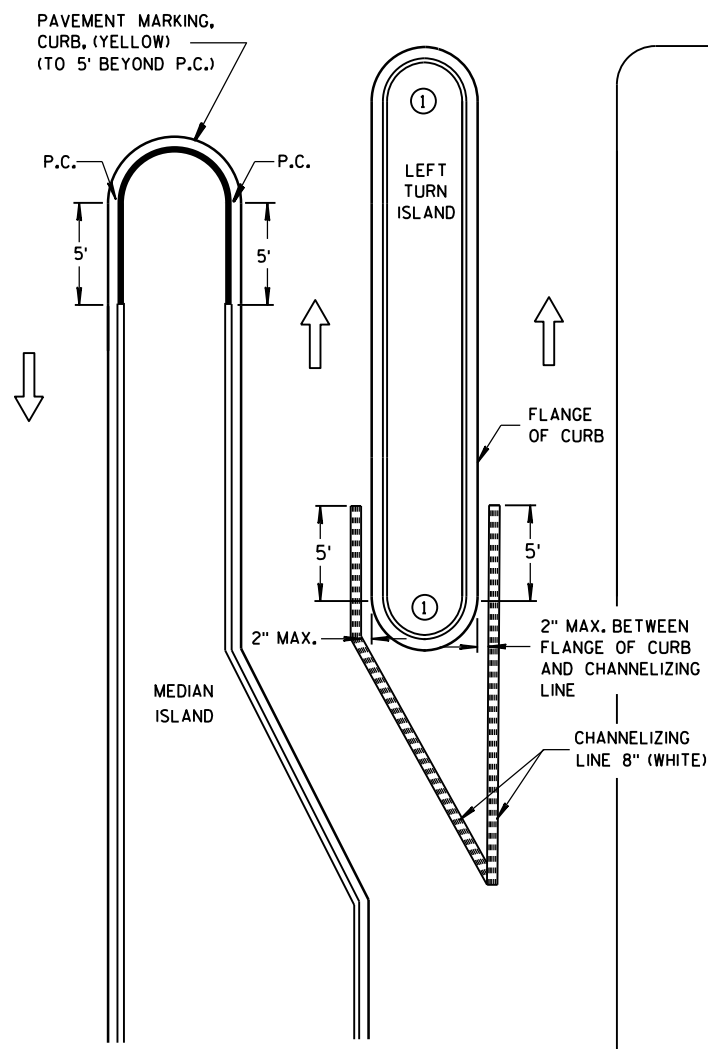


MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

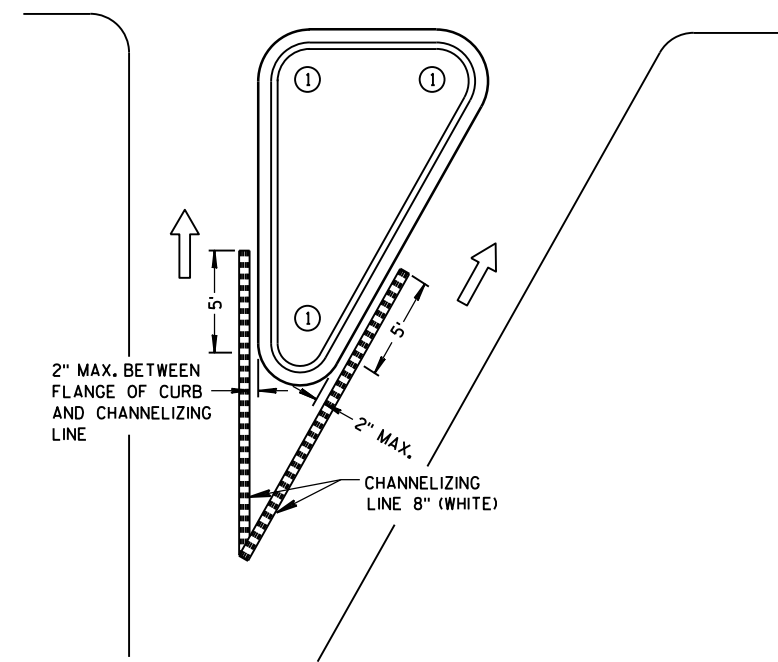
TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS



LEFT TURN & MEDIAN ISLAND

GENERAL NOTES

- DO NOT MARK CURB NOSES THAT SEPARATE LANES OF TRAFFIC TRAVELING IN THE SAME DIRECTION.
- WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.



RIGHT TURN ISLAND





LEGEND

- ISLAND NOSE MARKING
- CURB MARKING
- CORRUGATED MEDIAN MARKING
- DIRECTION OF TRAVEL

PAVEMENT MARKING (ISLANDS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

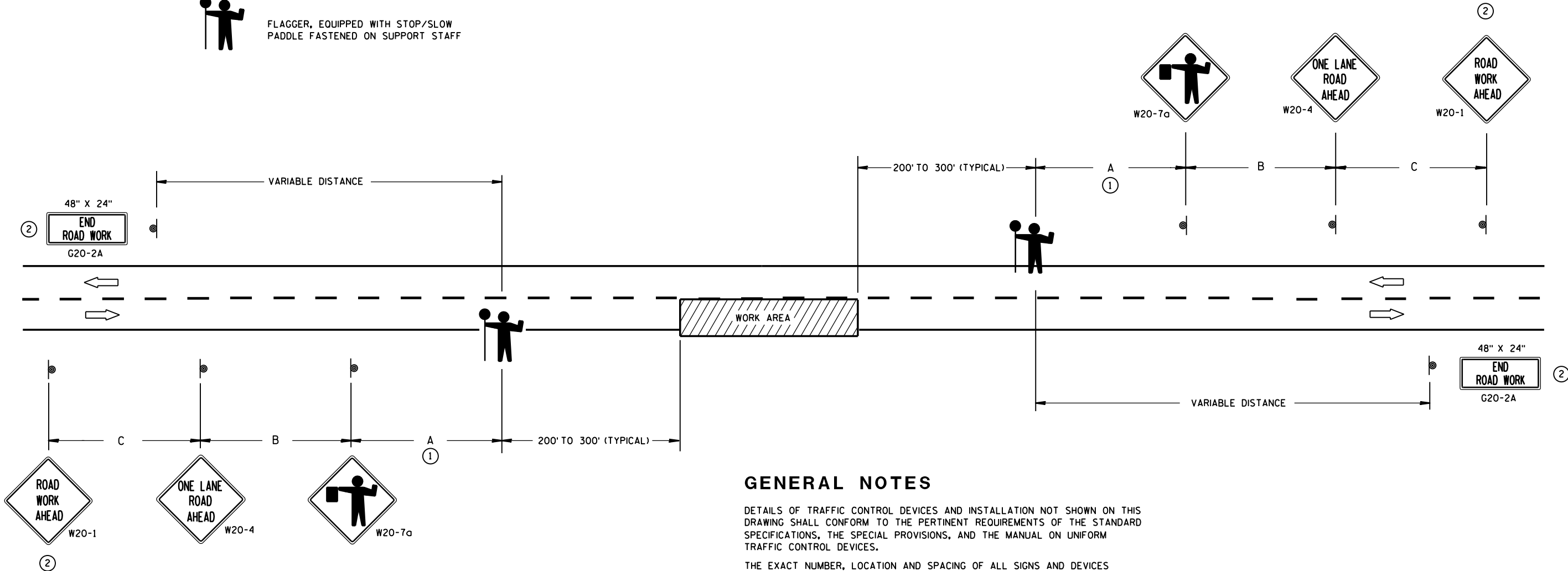
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

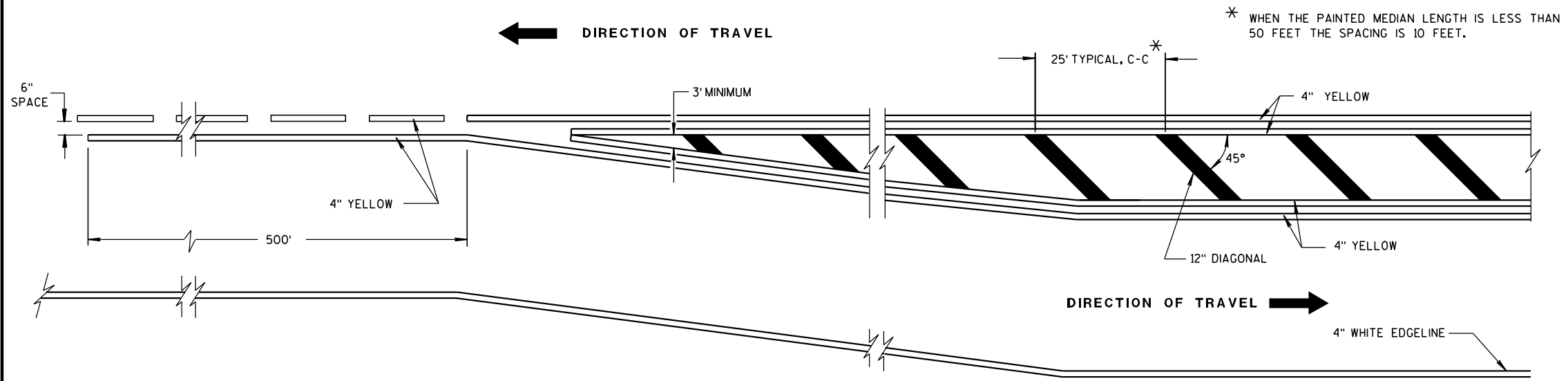
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

TRAFFIC CONTROL FOR LANE
CLOSURE (SUITABLE FOR
MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

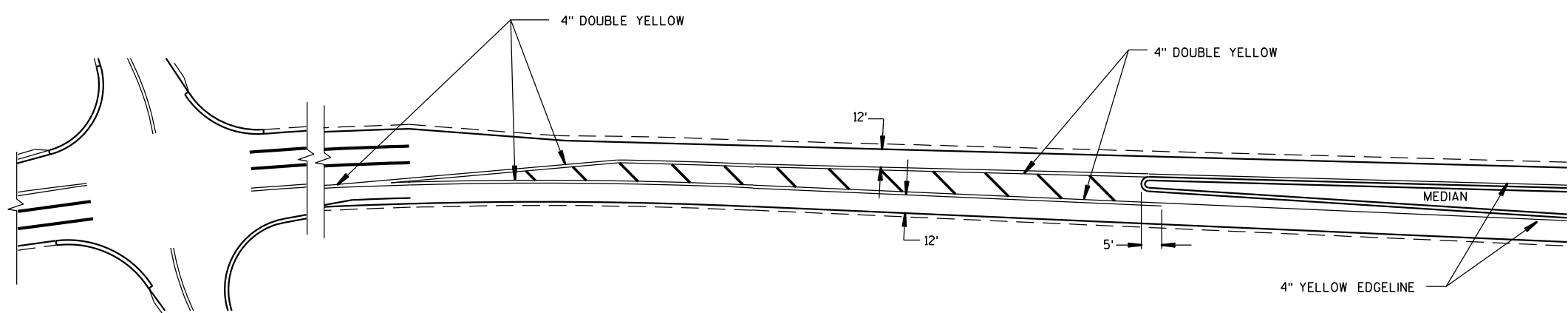
APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



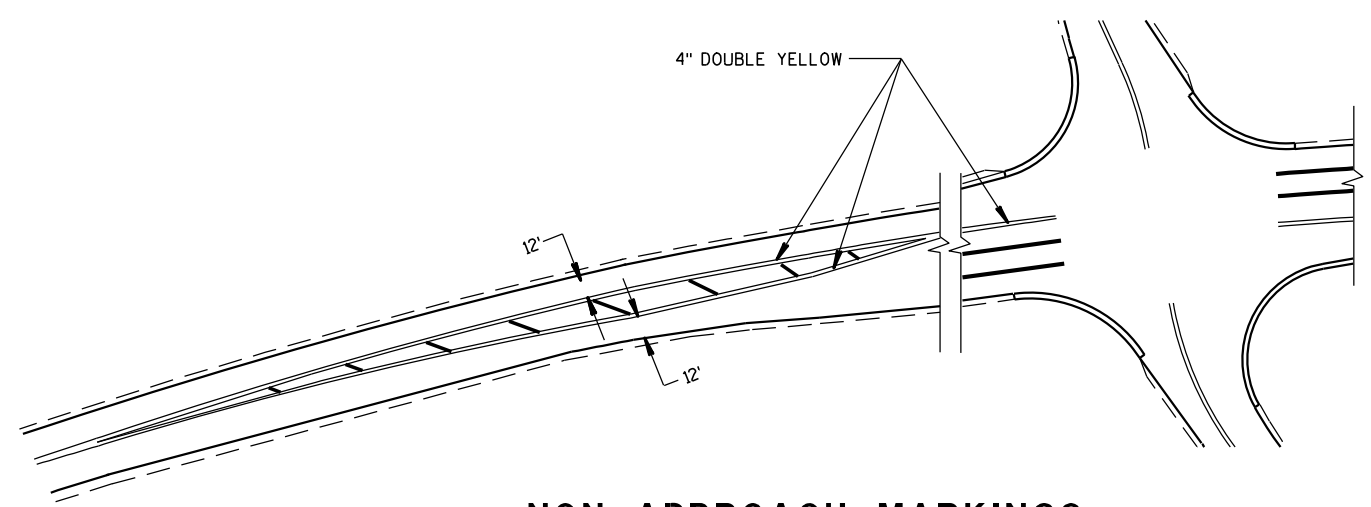
GENERAL NOTE

DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT WIDEST POINT.

MEDIAN ISLAND DETAIL



APPROACH MARKINGS FOR OTHER MEDIAN TYPES

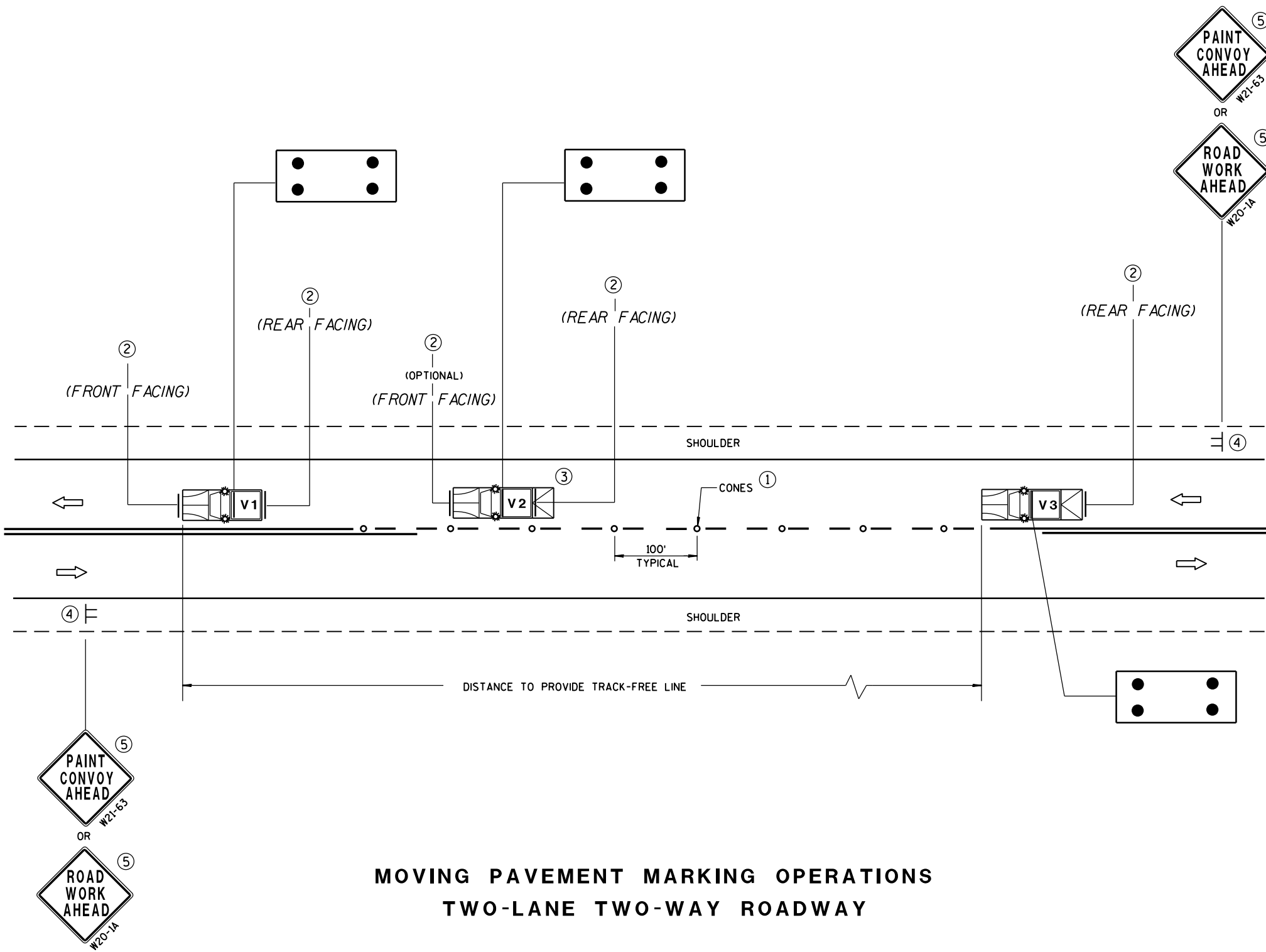


NON APPROACH MARKINGS

MEDIAN ISLAND MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED 2-5-09 DATE	/S/ Thomas N. Notbohm STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.



ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.






THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
 OR 
W21-64 W21-64
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

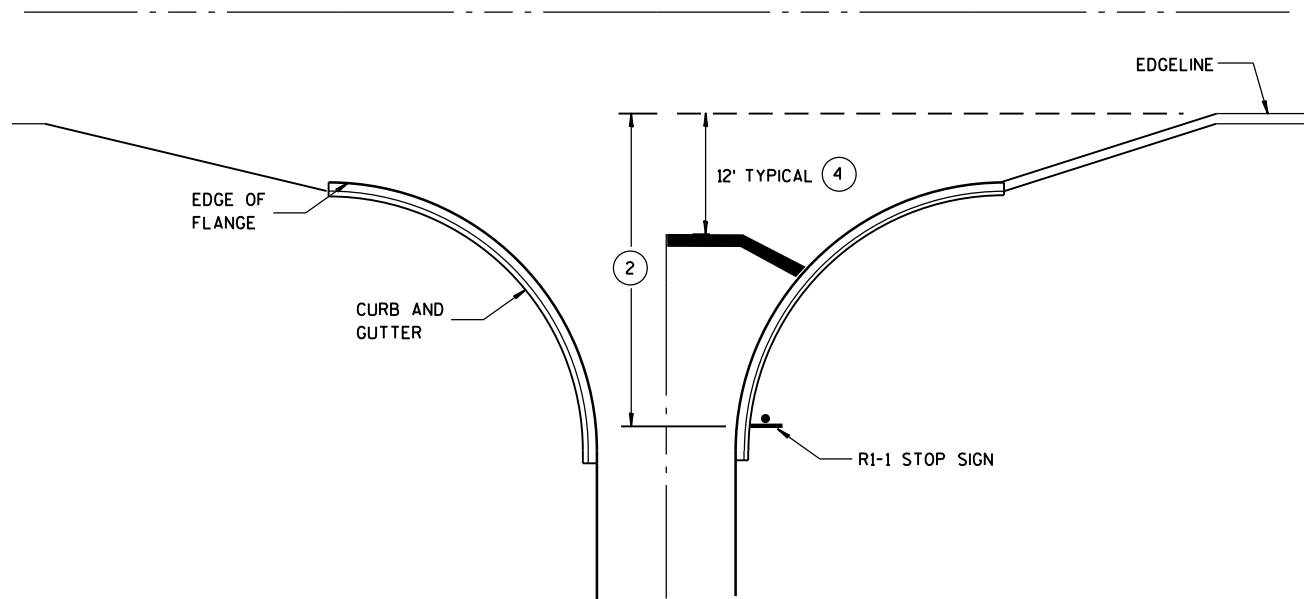
LEGEND

- V1** LEAD VEHICLE
- V2** SHADOW VEHICLE
- V3** TRAIL VEHICLE WITH TMA
-  **TMA** TRUCK-MOUNTED ATTENUATOR
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  CONES
-  FLASHING ARROW PANEL (CAUTION)

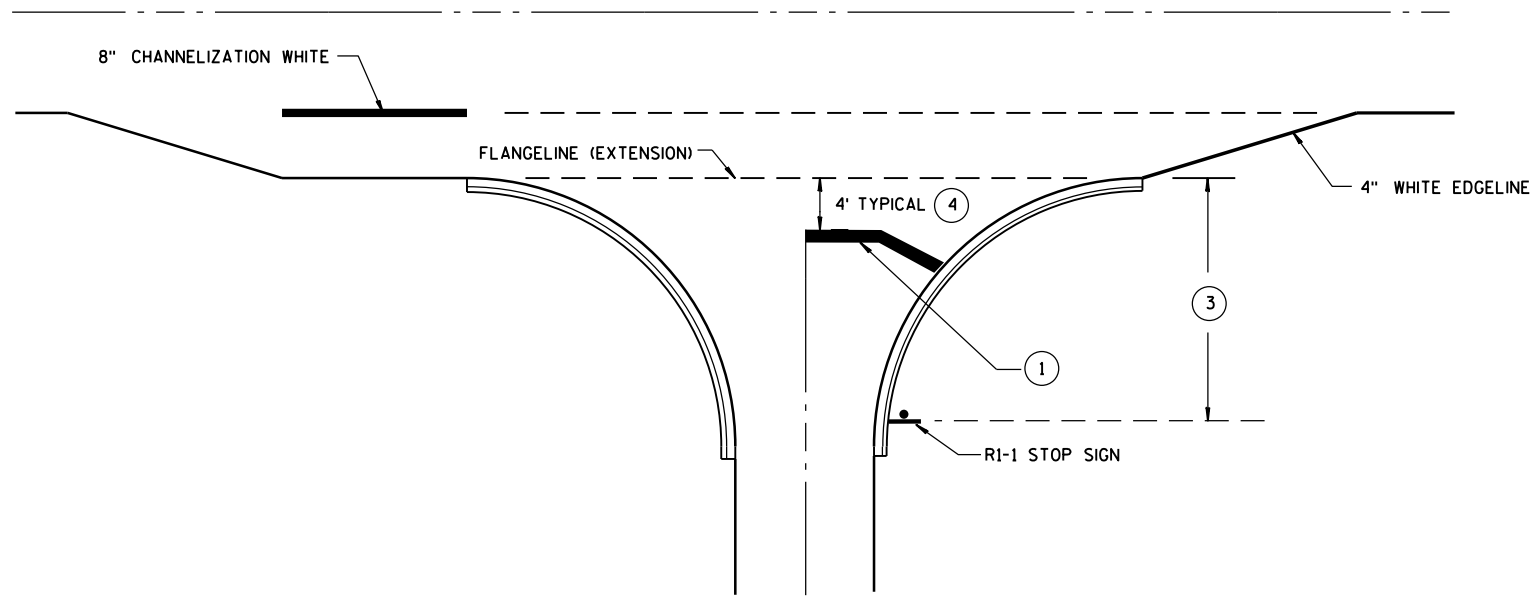
MOVING PAVEMENT MARKING
OPERATION
TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

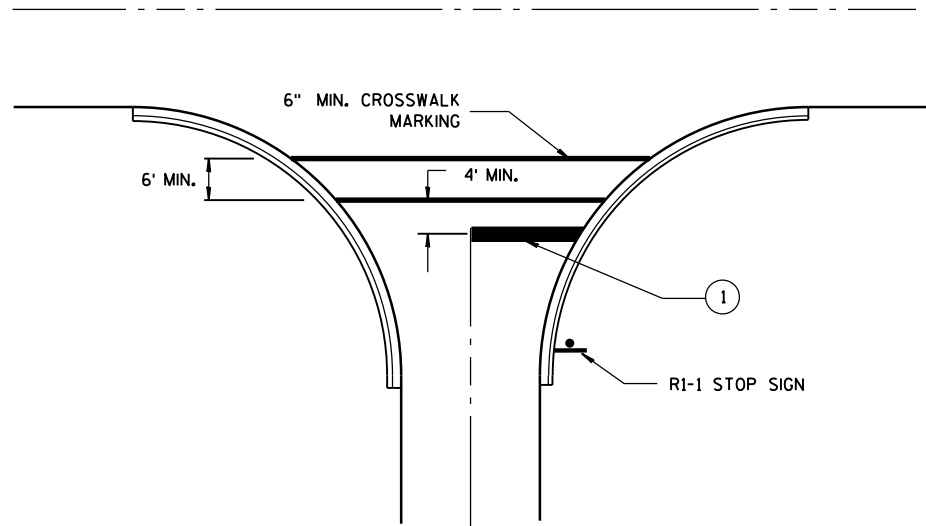
APPROVED
June 2016 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



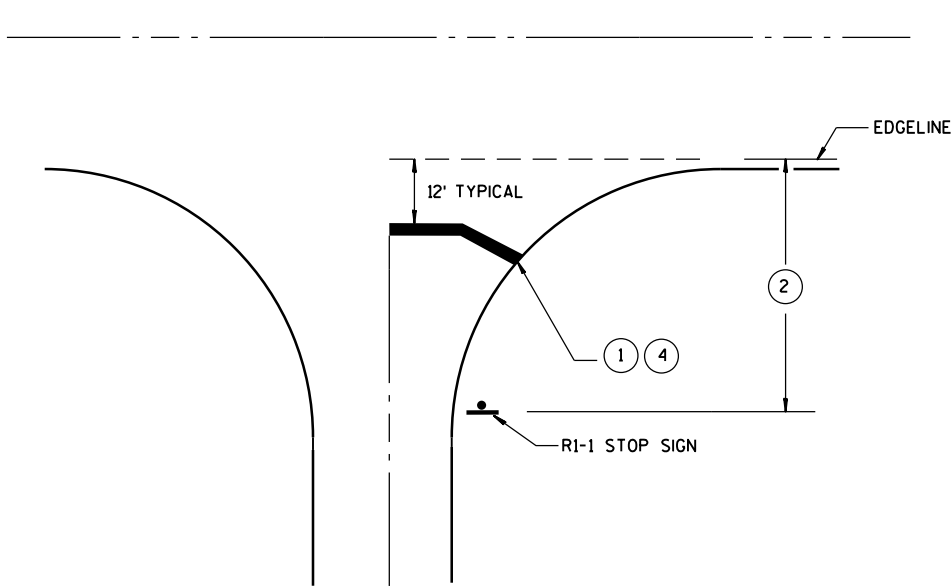
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

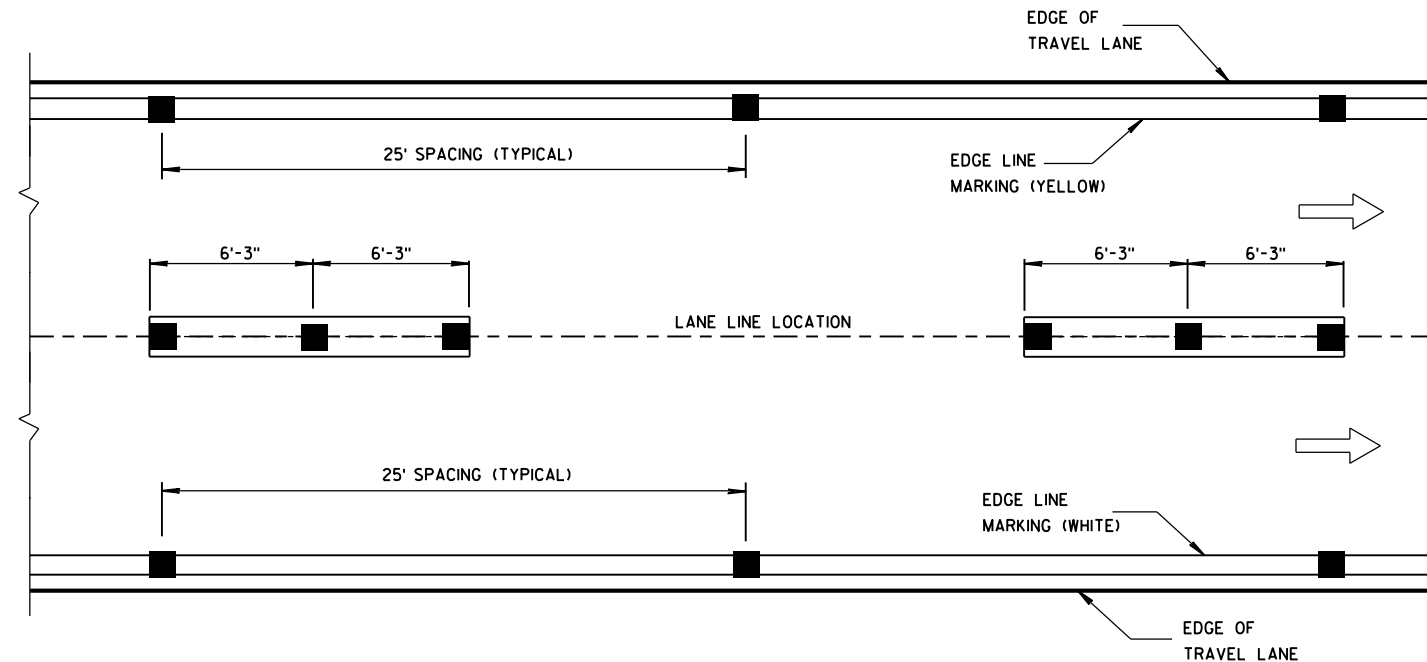
GENERAL NOTES

- 1 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- 2 IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE THAN NO STOP LINE IS REQUIRED.
- 3 IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- 4 MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

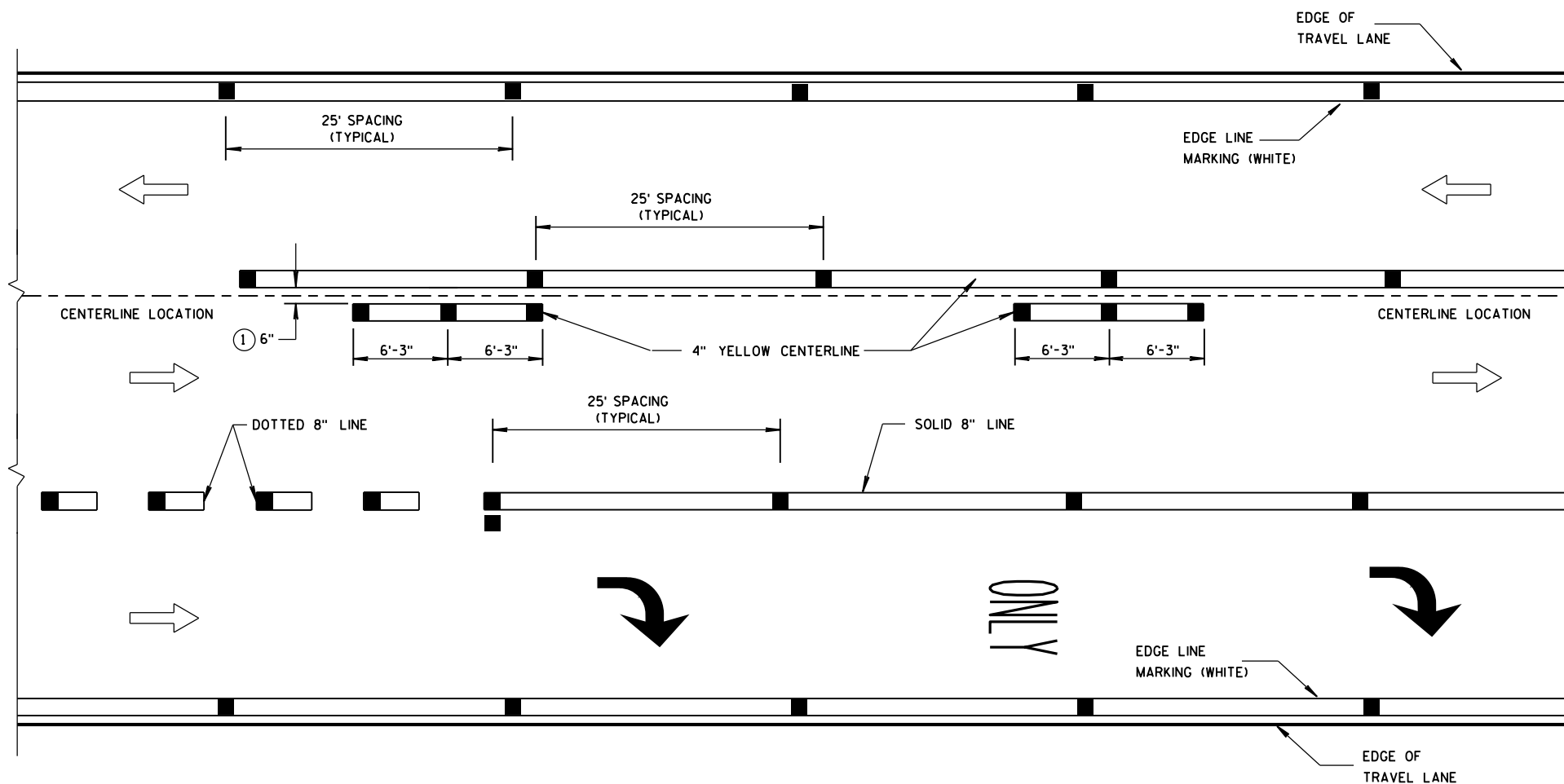
STOP LINE AND CROSSWALK
PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-18-2016 /S/ Matthew R. Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA



**LONGITUDINAL PLACEMENT
LANE LINE**



**LONGITUDINAL PLACEMENT
CENTERLINE AND 8-INCH CHANNEL LINE**

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

COLOR OF TEMPORARY RAISED PAVEMENT MARKERS, TYPE II SHALL MATCH THE COLOR OF THE MARKING THEY SUPPLEMENT.

PLACE 3 TEMPORARY RAISED PAVEMENT MARKERS EVENLY SPACED ON EACH 12.5 FOOT LINE.

PROVIDE SINGLE OR MULTI-COVER TEMPORARY RAISED PAVEMENT MARKERS AS SHOWN ON PLAN.

PLACE TEMPORARY RAISED PAVEMENT MARKERS, TYPE II ON EDGELINE TAPER POINTS AND END POINTS.

MARK T's ON PAVEMENT FOR RE-ESTABLISHING NO PASSING ZONES.

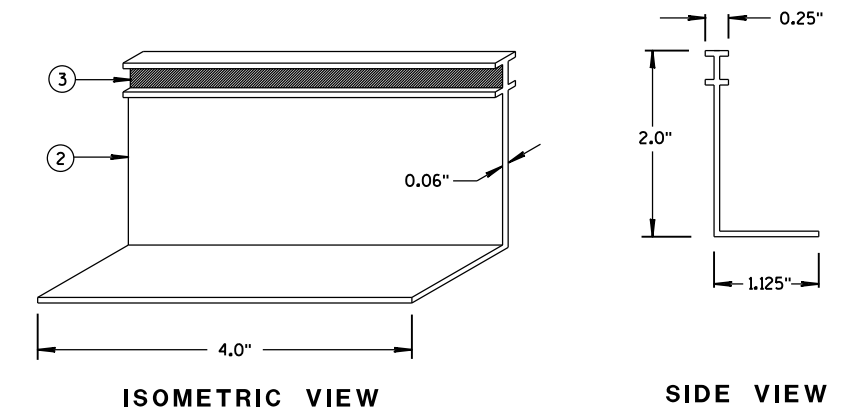
- ① FOR DOUBLE SOLID YELLOW, PLACE THE MARKERS DIRECTLY ACROSS FROM EACH OTHER, WITH A 6 INCH SPACE BETWEEN THEM.
- ② MARKERS SHALL BE OF POLYURETHANE MATERIAL.
- ③ MARKERS SHALL HAVE A MINIMUM SIZE REFLECTIVE SURFACE OF 4 INCH WIDTH X 0.25 INCH HEIGHT.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

LEGEND

■ TEMPORARY RAISED PAVEMENT MARKER, TYPE II



**TEMPORARY
RAISED PAVEMENT MARKER**

STANDARD APPLICATION FOR TEMPORARY RAISED PAVEMENT MARKERS, TYPE II	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-28-2016 DATE	/S/ Matthew R. Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

TABLE A

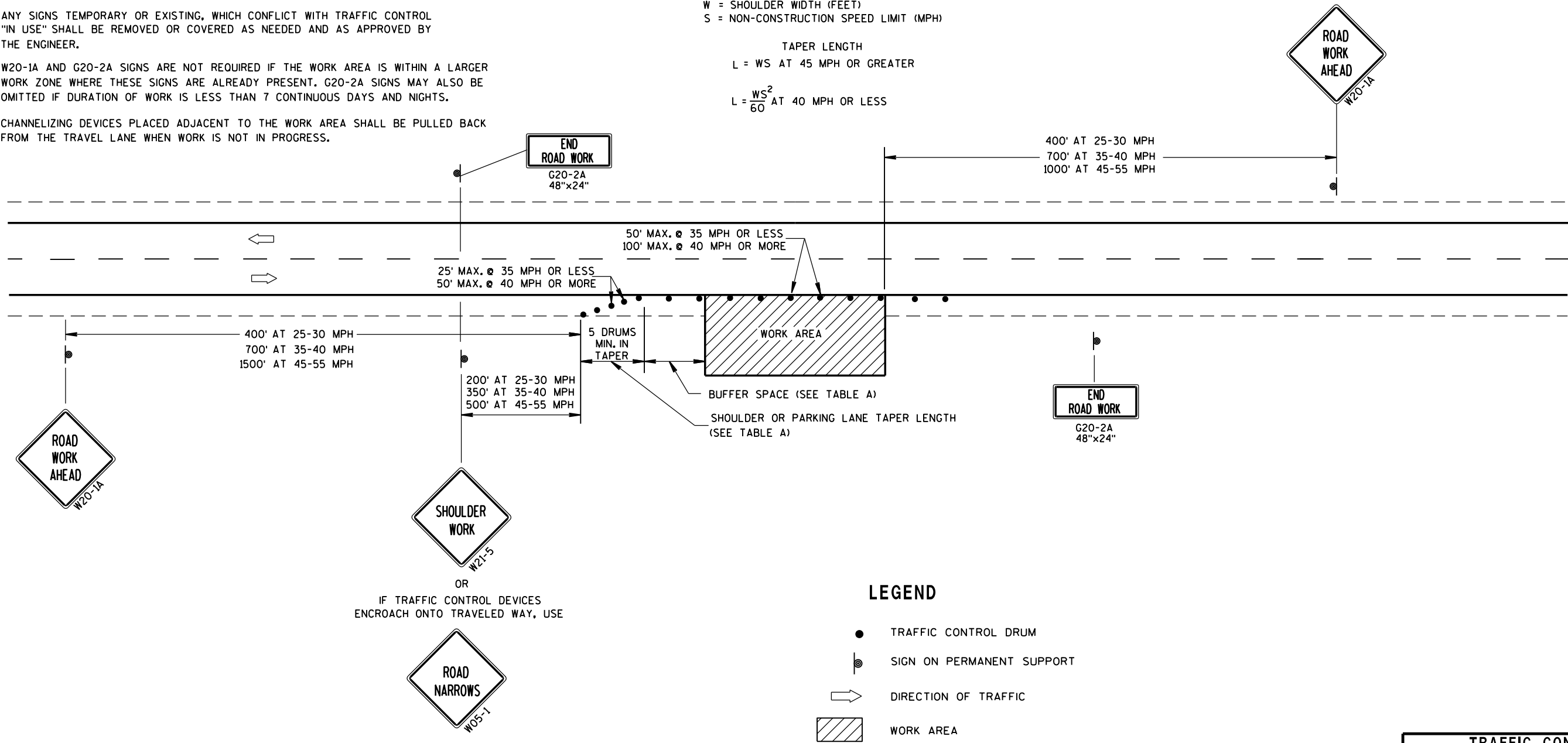
SHOULDER TAPER LENGTH (FEET)					BUFFER SPACE (FEET)
S \ W	4	6	8	10	
30	20	30	40	50	200
35	30	45	55	70	250
40	40	55	75	90	305
45	60	90	120	150	360
50	70	100	135	170	425
55	75	110	150	185	495

W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH
L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = $\frac{1}{3}L$



LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

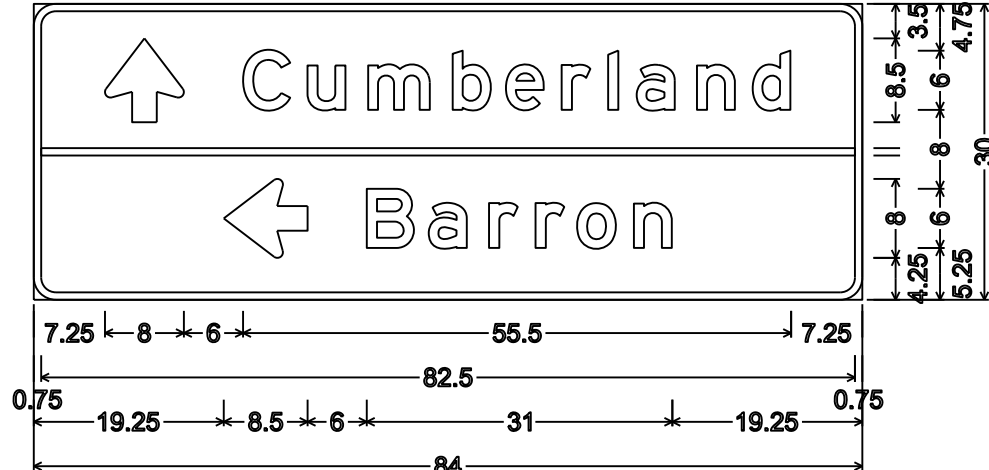
TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 14, 2015 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	



D2-1; 2.250" Radius, 0.750" Border



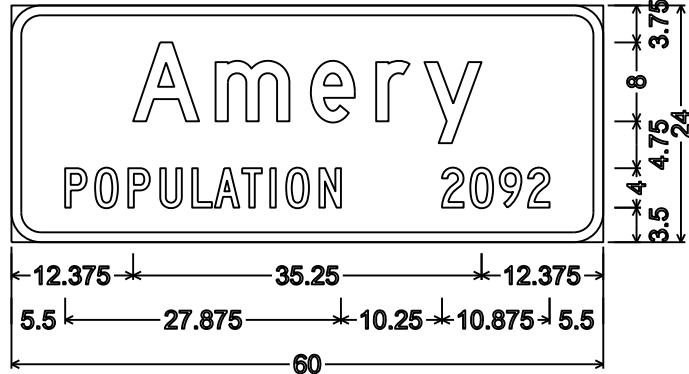
D2-1; 2.250" Radius, 0.750" Border



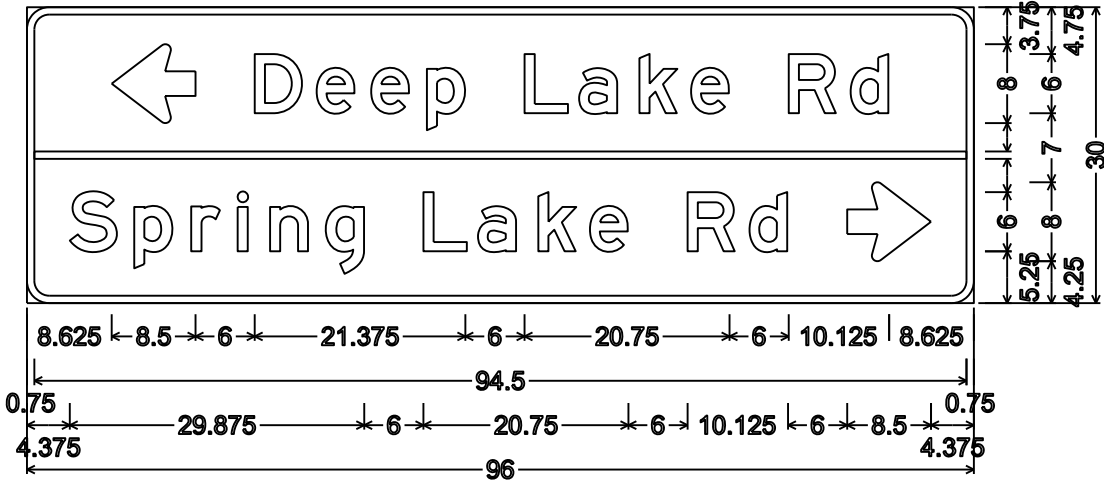
D1-2; 2.250" Radius, 0.750" Border



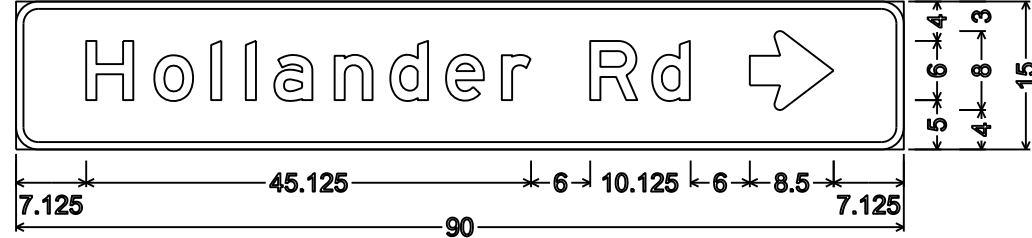
D1-1; 2.250" Radius, 0.750" Border



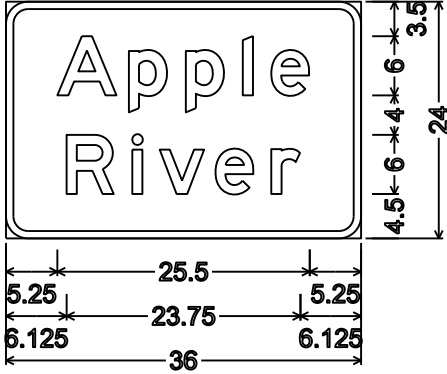
I2-3;
3.000" Radius, 1.000" Border,
"Amery" D; "POPULATION" C; "2092" C



D1-2; 2.250" Radius, 0.750" Border

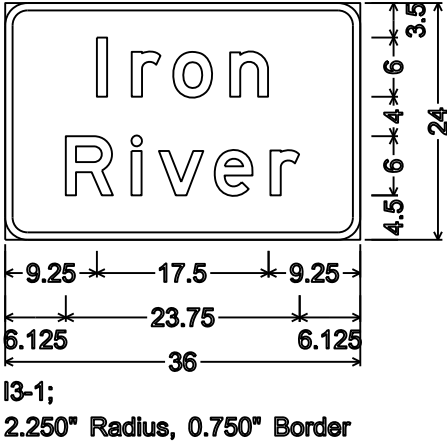


D1-1; 2.250" Radius, 0.750" Border

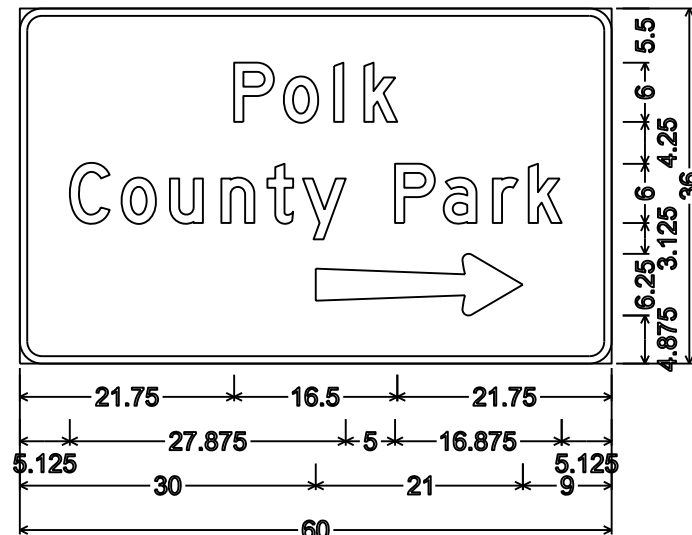


I3-1;
2.250" Radius, 0.750" Border

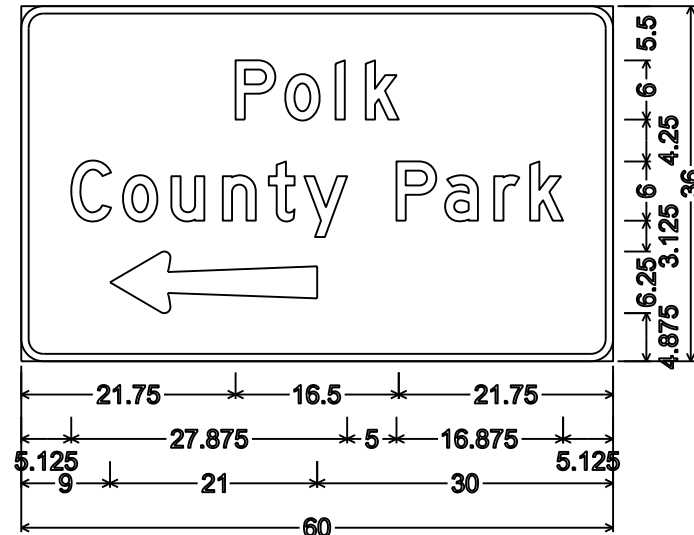
- NOTES
1. All Signs Type II - Type H Reflective
 2. Color:
Background - Green except as noted
Message - White
 3. Message Series - E except as noted



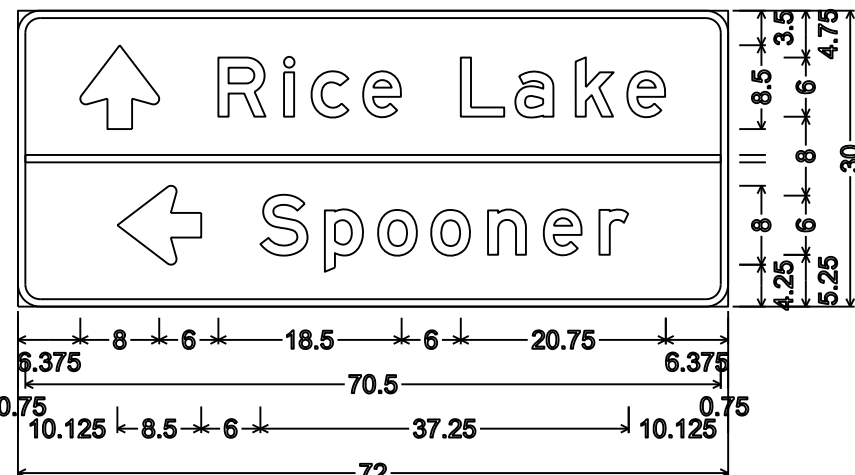
I3-1;
2.250" Radius, 0.750" Border



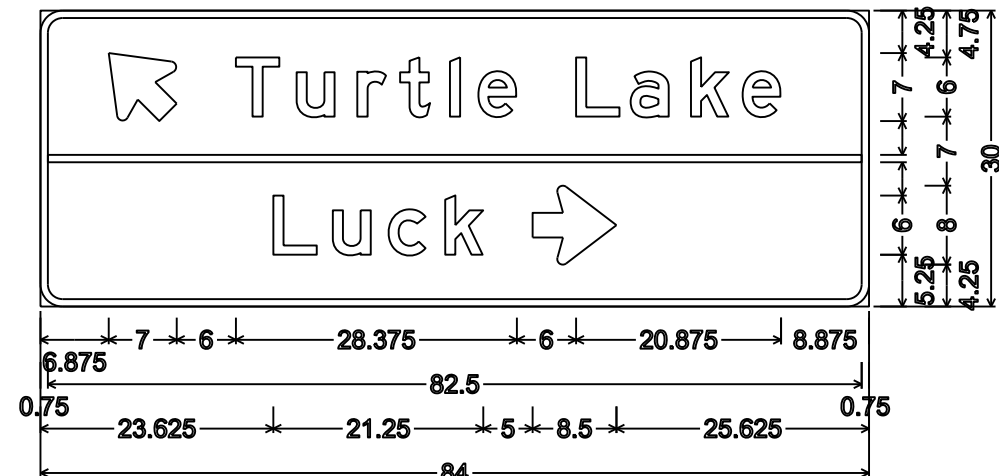
D7-68R;
2.250" Radius, 0.750" Border, White on Brown;
"Polk" D; "County" D; "Park" D



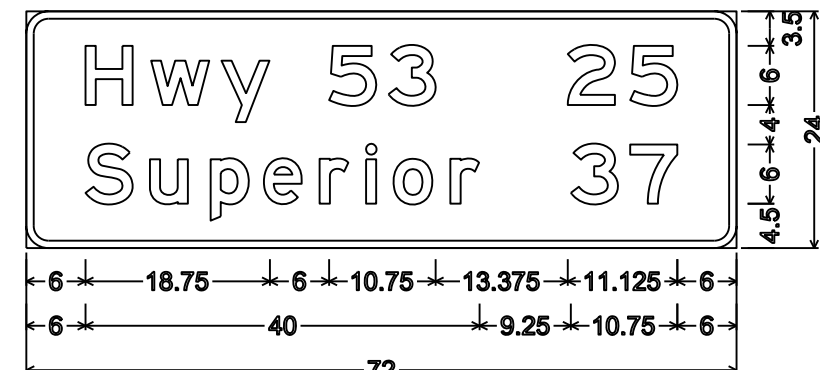
D7-68L;
2.250" Radius, 0.750" Border, White on Brown;
"Polk" D; "County" D; "Park" D



D1-2; 2.250" Radius, 0.750" Border



D1-2; 2.250" Radius, 0.750" Border

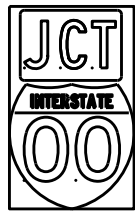


D2-2; 2.250" Radius, 0.750" Border

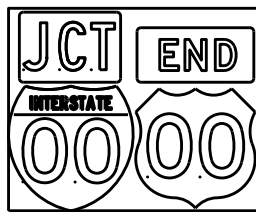
NOTES

1. All Signs Type II - Type H Reflective
2. Color:
Background - Green except as noted
Message - White
3. Message Series - E except as noted

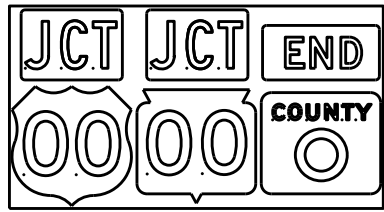
TYPICAL ASSEMBLIES



J1-1



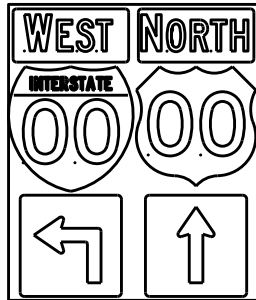
J1-2



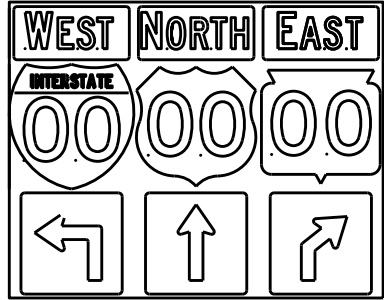
J1-3



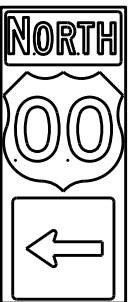
J2-1



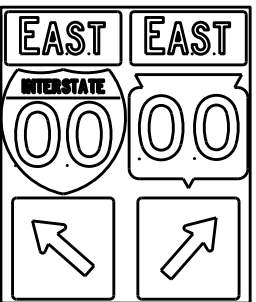
J2-2



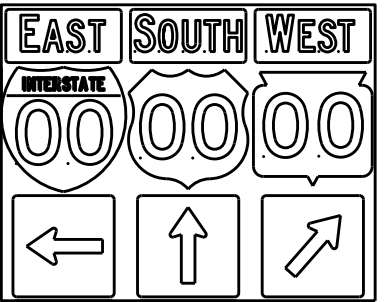
J2-3



J3-1



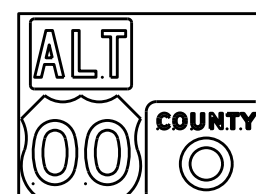
J3-2



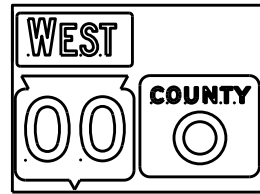
J3-3



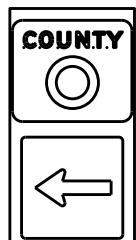
J4-1



J4-2



J4-2



J13-1



J12-1



J32-1



J33-1



J23-1

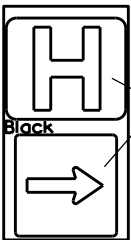


J22-1



JV

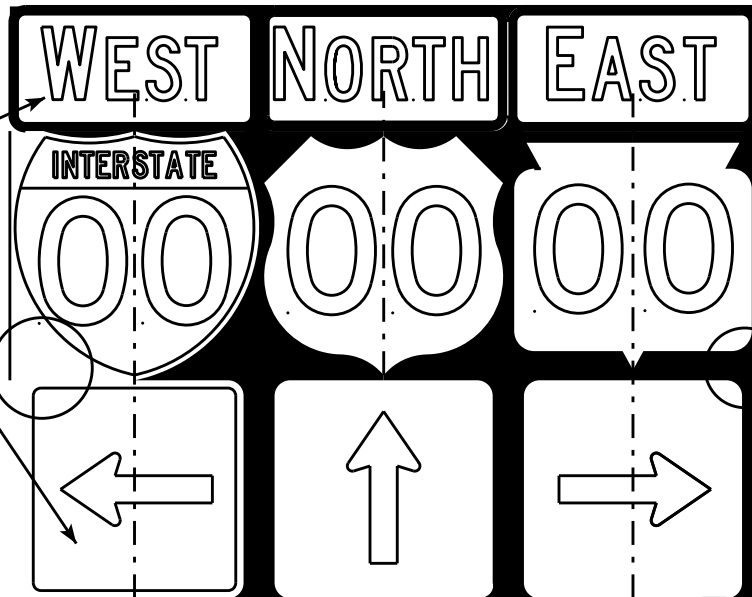
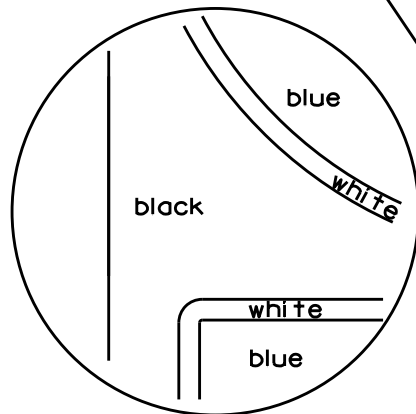
(Typical Vertical J-Assembly
See Note 10 and 11)



JH-1

Blue Background

[blue background
with interstate]



[black background]

ROUTE MARKERS & COMPONENTS
IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/06/14 PLATE NO. A2-1S.8

NOTES

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Black Non-reflective
Message - see Note 5
3. Message Series - See Note 5
4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
10. All Vertical J Assemblies are given a Sign Code of JV
11. For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

PROJECT NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A21S.DGN

PLOT DATE : 06-FEB-2014 14:10

PLOT BY : mscs.ja

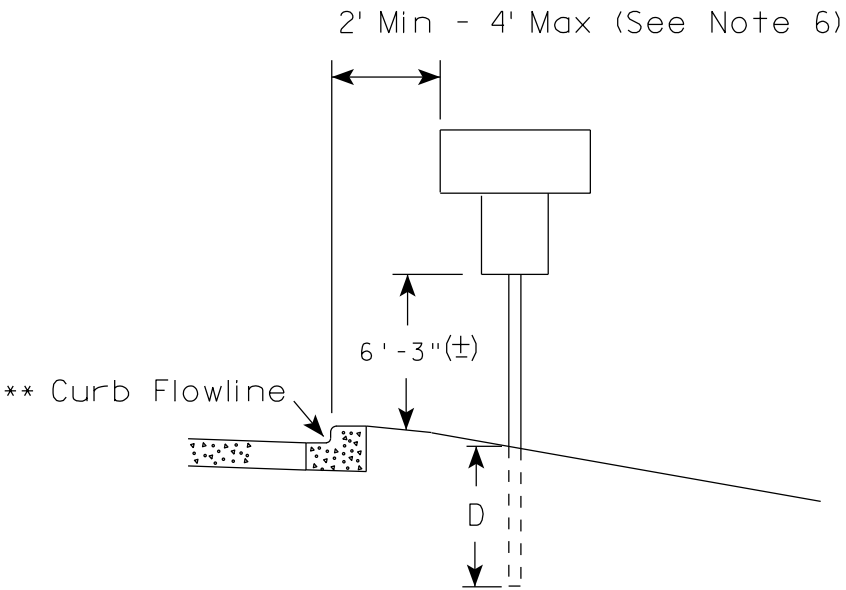
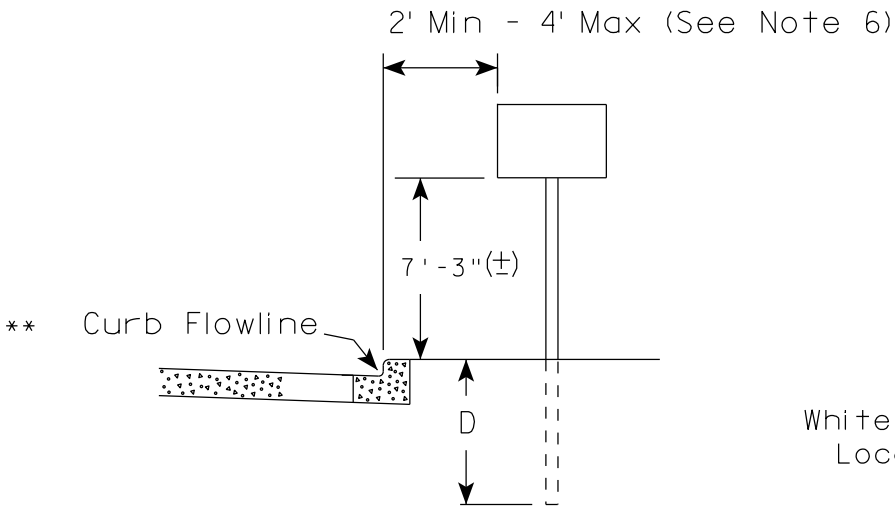
PLOT NAME :

SHEET NO:

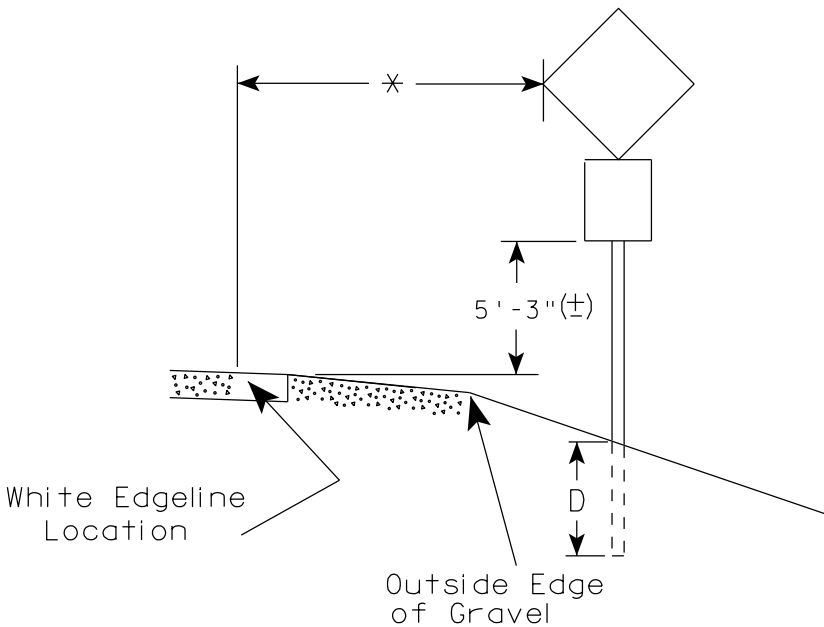
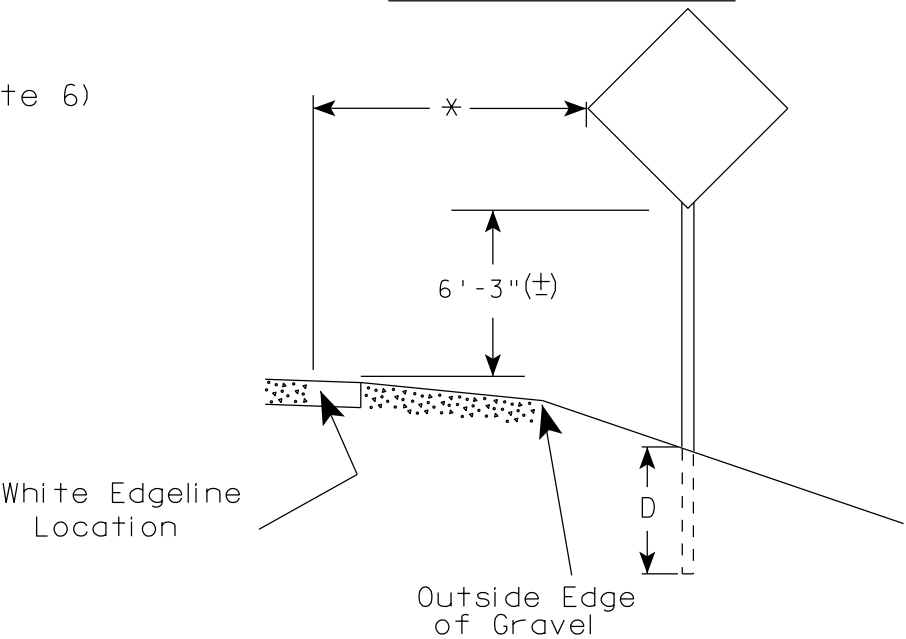
E

WISDOT/CADDs SHEET 42

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

×× The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

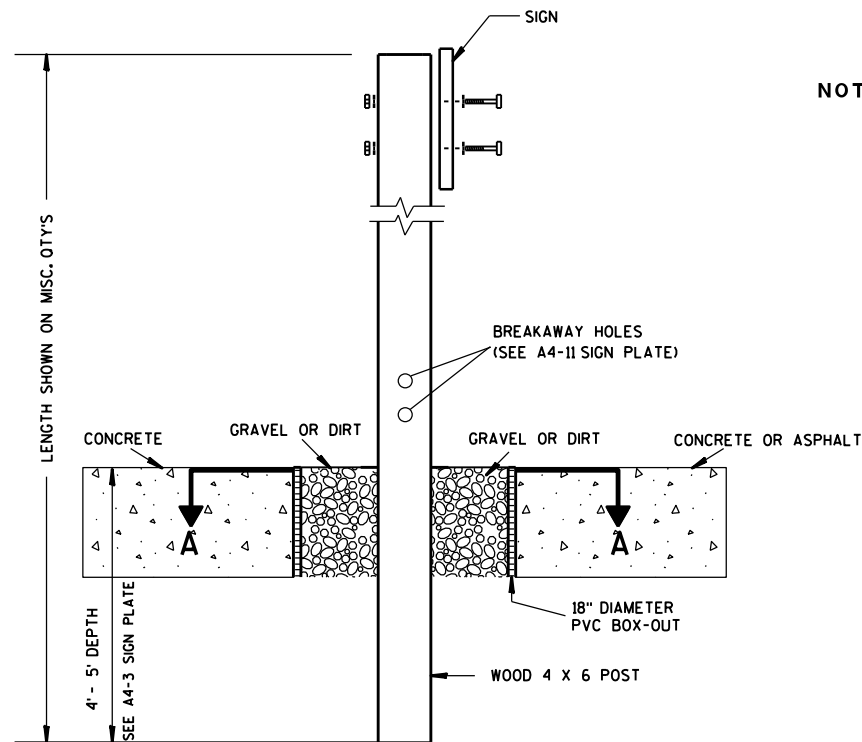
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

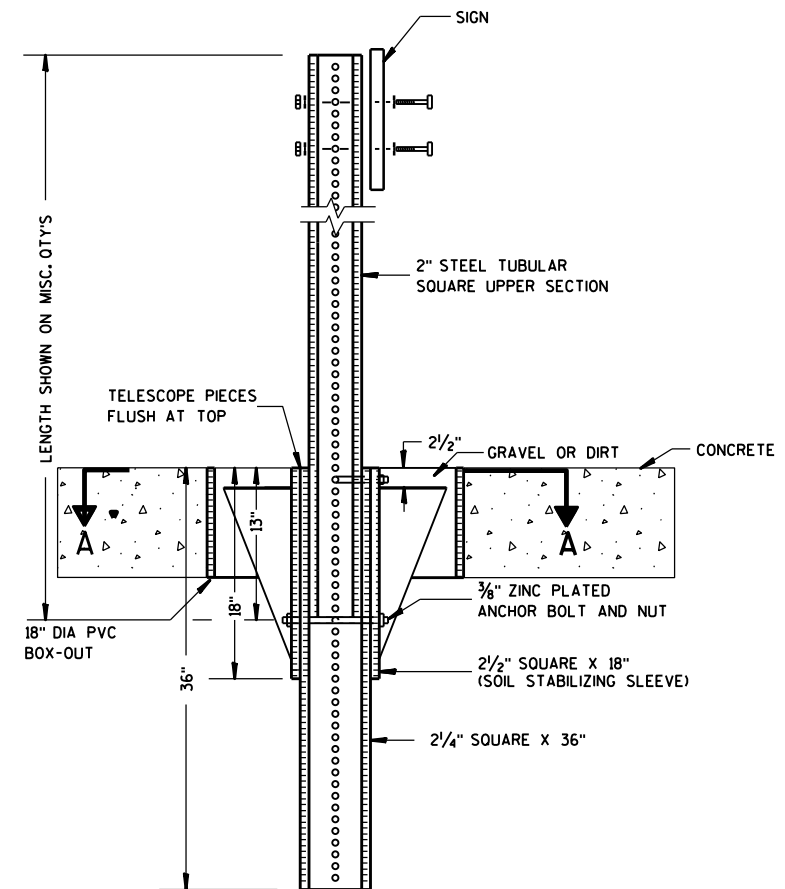
DATE 7/23/15 PLATE NO. A4-3.20



ELEVATION VIEW

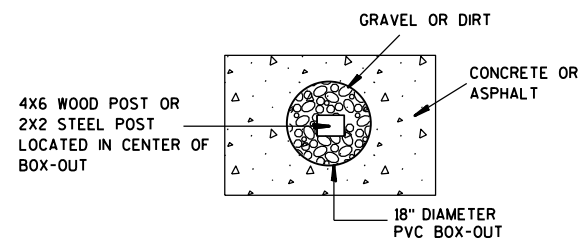
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

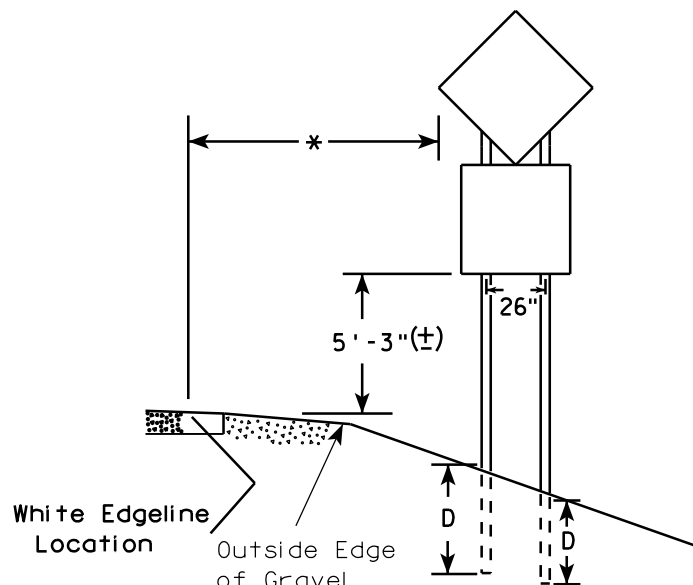
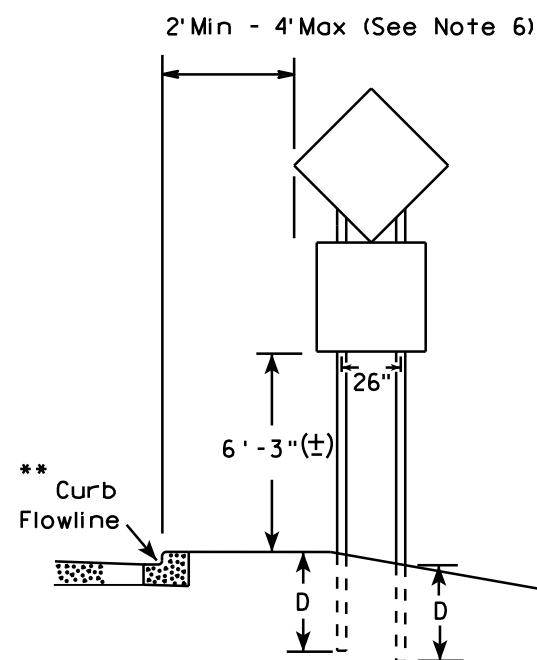
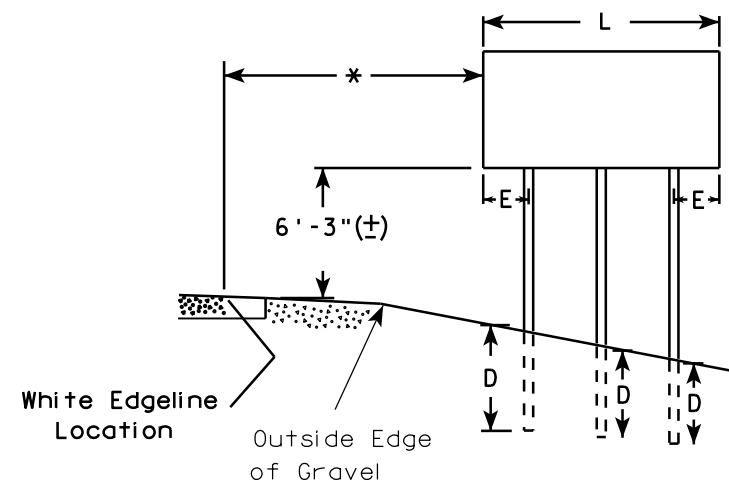
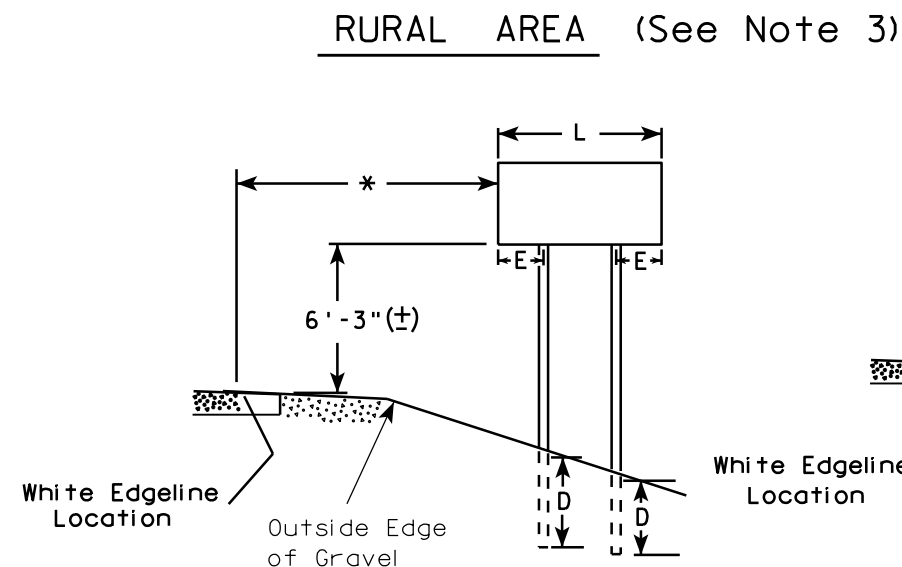
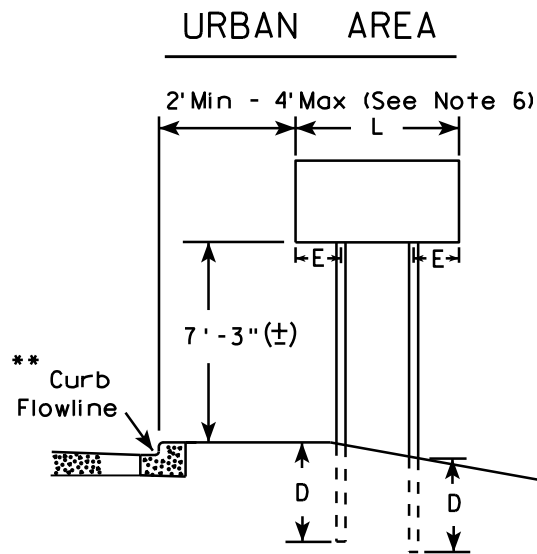
FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1



48" DIAMOND WARNING SIGN

48" DIAMOND WARNING SIGN

- GENERAL NOTES**
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

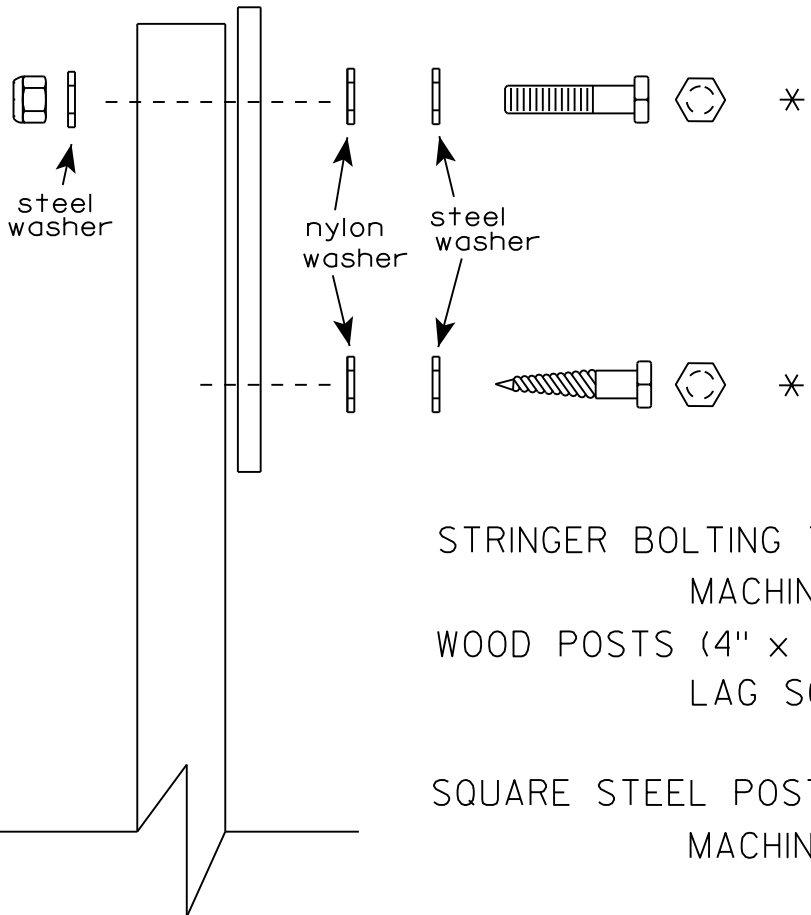
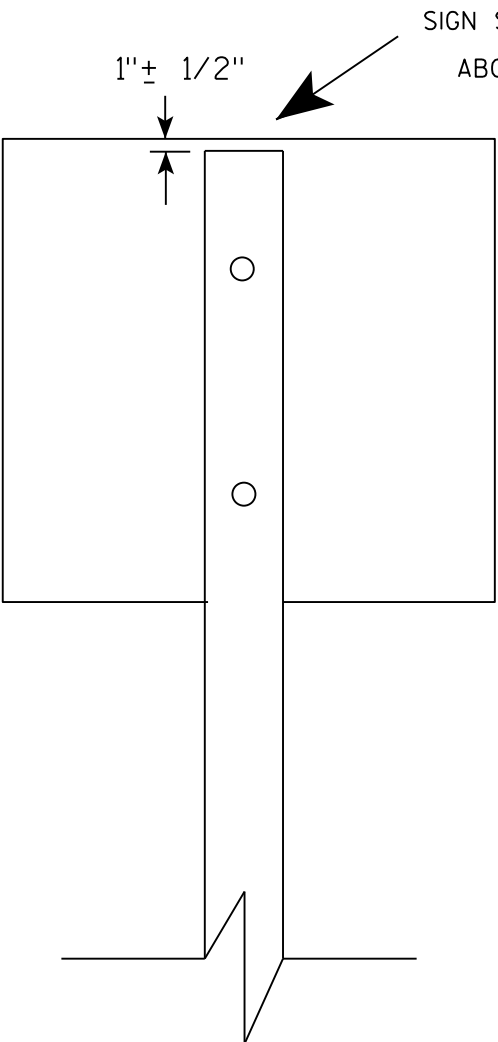
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-4.14



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

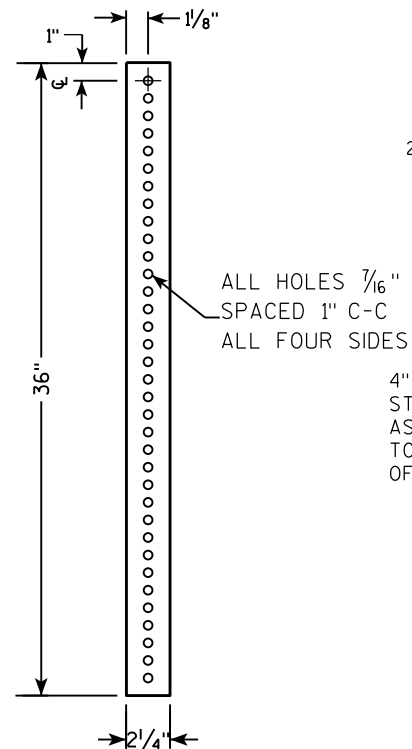
Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 4" or 4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

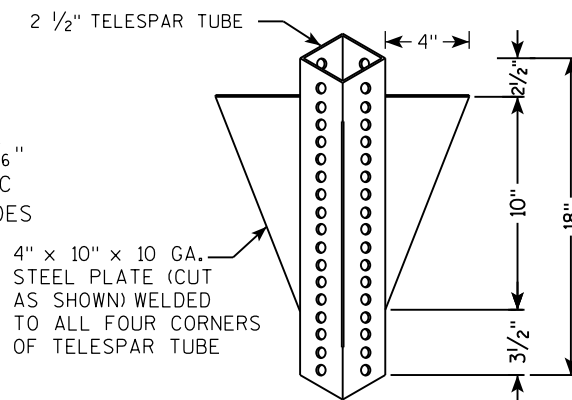
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE <u>8/11/16</u>	PLATE NO. <u>A4-8.8</u>

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**

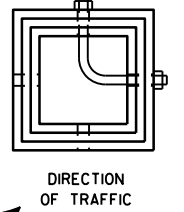


**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



LENGTH SHOWN ON MISC. QTY'S
 18" DIA SCHEDULE 40 PVC BOX-OUT
 TELESCOPE PIECES FLUSH AT TOP
 36"
 18"
 13"
 2 1/2"
 2 1/4" SQUARE X 36"
 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)
 3/8" ZINC PLATED ANCHOR BOLT AND NUT
 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT
 ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES
 2" STEEL TUBULAR SQUARE UPPER SECTION
 SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL
 SIGN
 2 1/2" GRAVEL OR DIRT

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT →



SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthieu R. Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

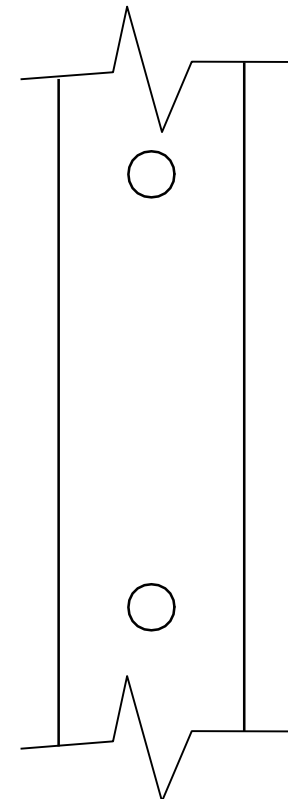
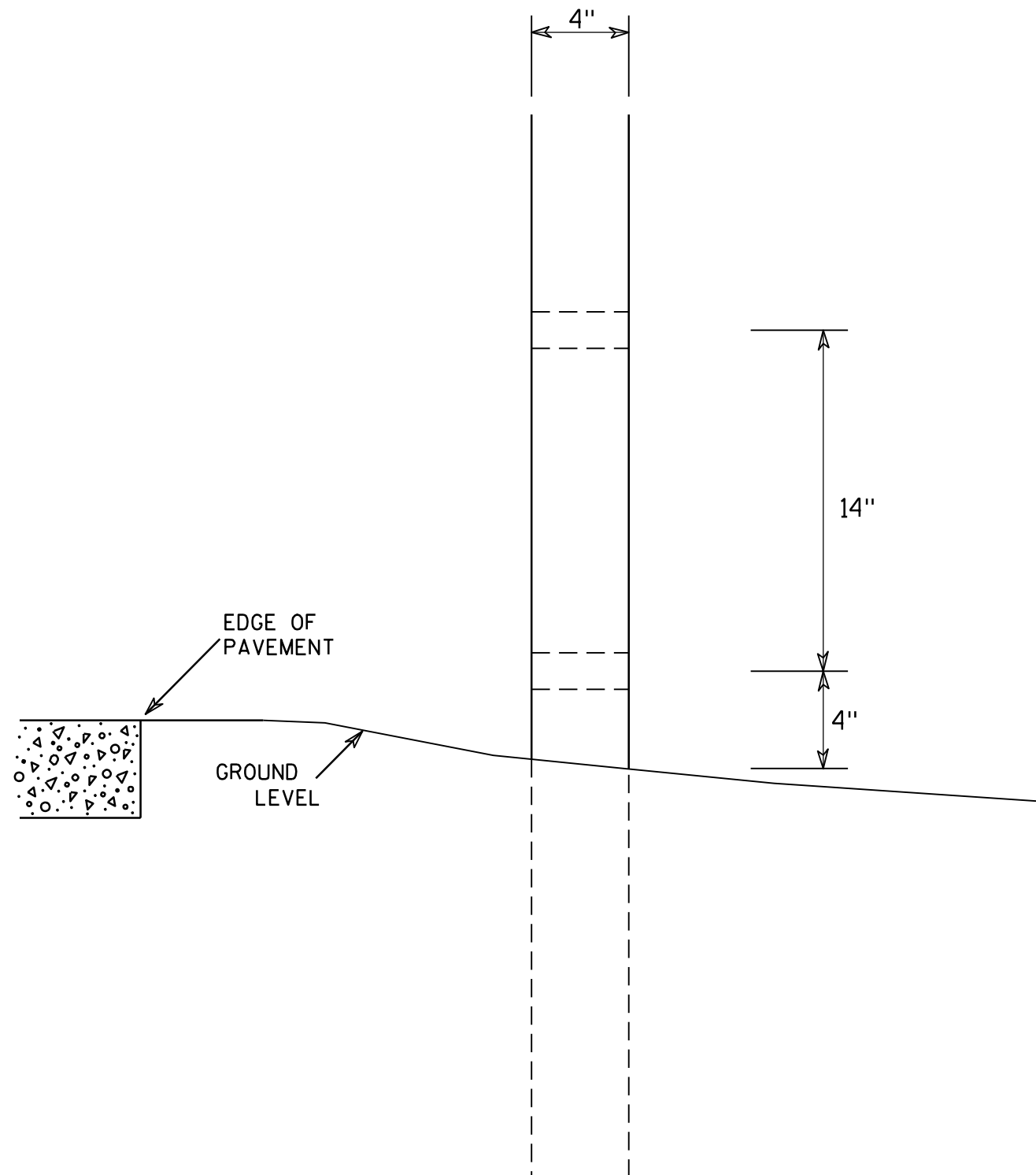
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

11



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1 1/2" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

7

Metric equivalent
for this sign is:

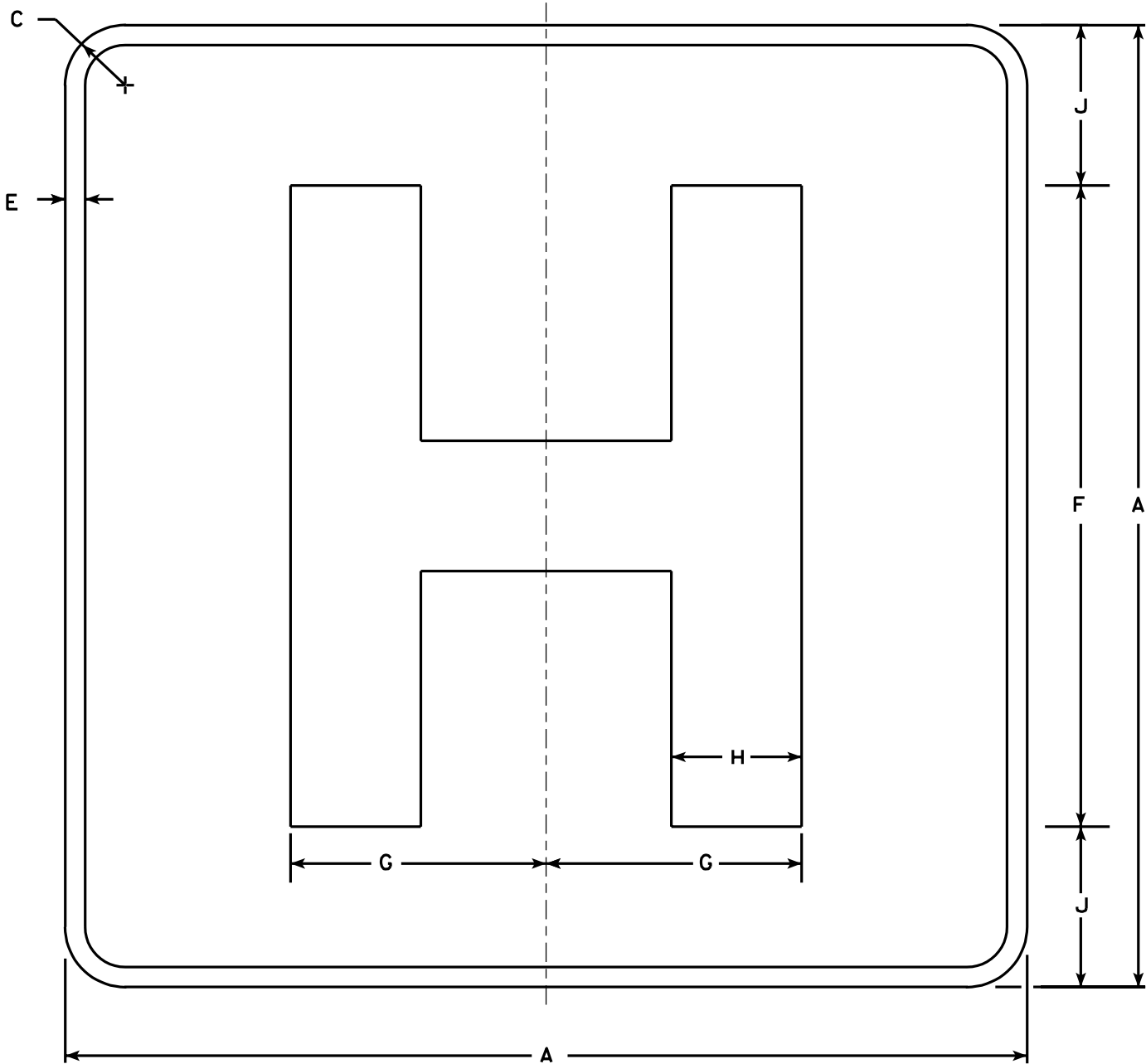
SIZE	
1	450 mmX 450 mm
2	600 mmX 600 mm
3	900 mmX 900 mm
4	X
5	X

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8		1/2	12	4 3/4	2 3/8		3																	4.0
2	24		1 1/2		1/2	16	6 3/8	3 1/4		4																	4.0
3	36		2 1/4		3/4	24	9 1/2	4 7/8		6																	9.0
4																											
5																											

PROJECT NO:

SHEET NO:

E



D9-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Blue
Message - White - Type H Reflective
3. Message Series - E Modified
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

STANDARD SIGN
D9-2

WISCONSIN DEPT OF TRANSPORTATION

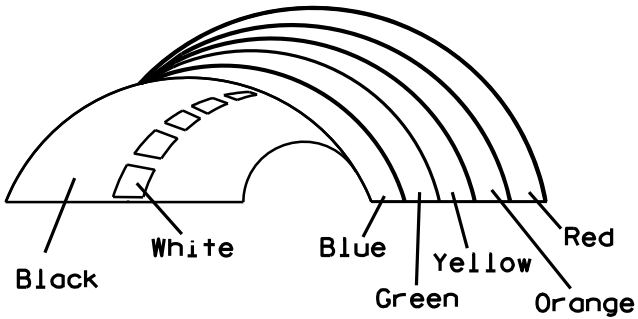
APPROVED
Chester J. Spang
for State Traffic Engineer

DATE 1/15/02 PLATE NO. D9-2.4



* VARIES

Background Colors of Symbol*



*1/4" Black Border between each color of rainbow and border of rainbow

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - (See Note 5)
3. Message Series - (See Note 6)
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Border - Blue
Line 1 - Red
Line 2 - Black
Line 3-5 - Blue
6. Line 1 - Dutch 8011L
Line 2 - Series E
Line 3-5 - Series C
7. Contractor shall provide and install a new post bracket in accordance with the I55-56B sign detail.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	36	1 1/2	1/2	5/8	3	2	3 1/2	2 7/8	1	8	2 1/8	11 1/4	11 1/8	9 3/8	1 1/4		3/4	12 5/8	7 1/2							7.5
3																											
4																											
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

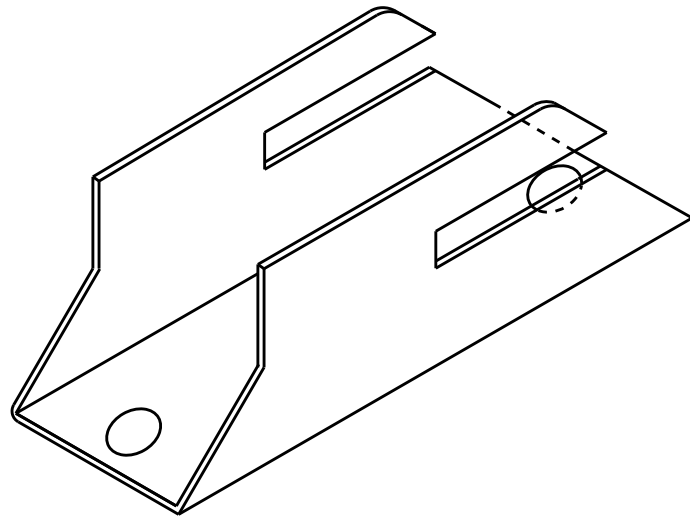
STANDARD SIGN
I55-56

WISCONSIN DEPT OF TRANSPORTATION

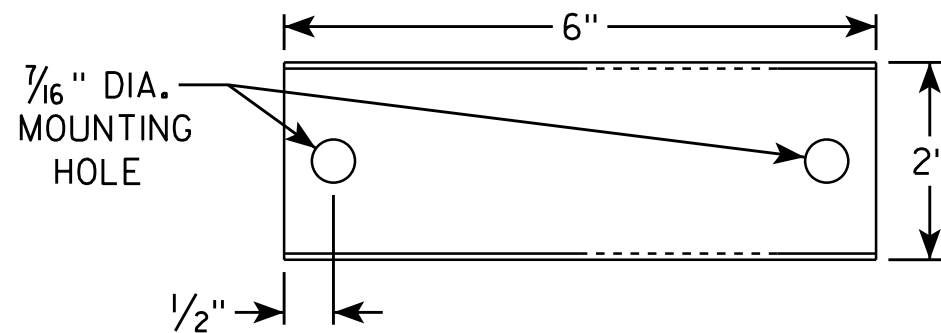
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/27/11 PLATE NO. I55-56.3

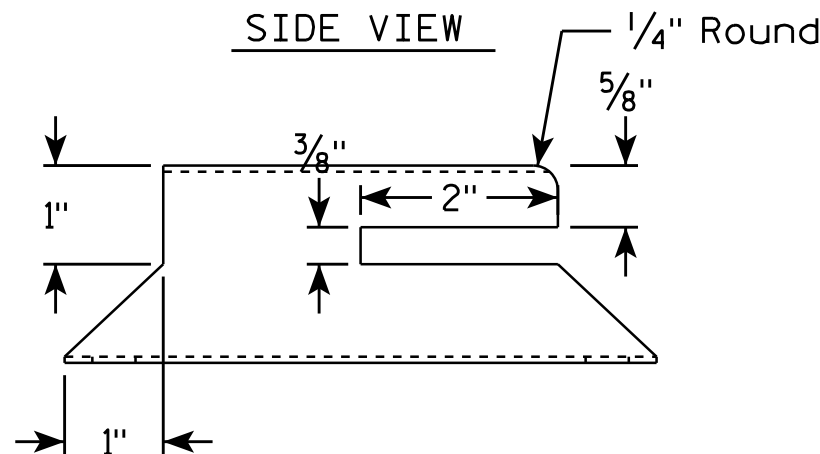
ISOMETRIC VIEW



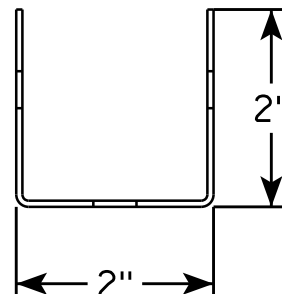
TOP VIEW



SIDE VIEW



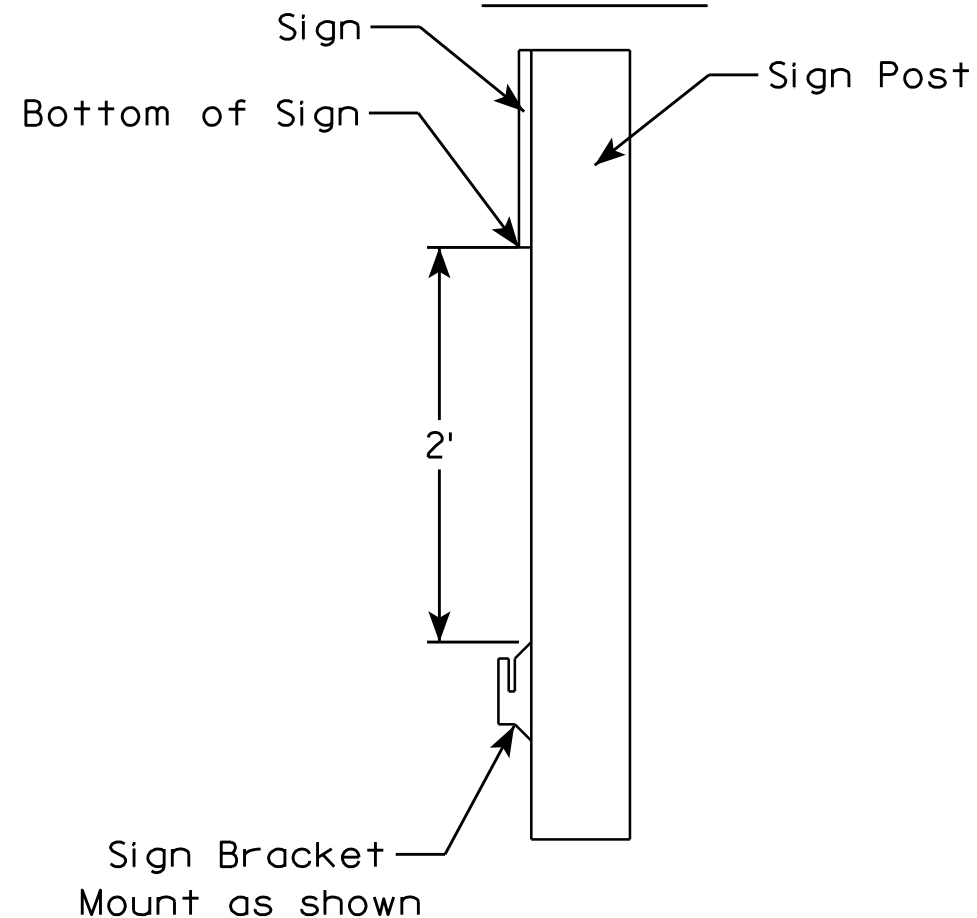
END VIEW



NOTES

1. Must be capable of permanent attachment to a wood or steel channel sign post utilizing the fastening hardware specified on the A4-8 sign plate.
2. Shall be entirely primed and painted with two coats of a black powder coated enamel paint.
3. Shall be made with 12 gauge steel, and incorporate no welds, no hinged components, no threaded lock-type components, and no parts which are loose or can be separated from the main body.
4. Shall have rounded edges with at least 1/8" radii.
5. Shall not have unrounded and uncoated metal edges which can contact the back surface of the roll-up sign.
6. Top of bracket shall be mounted 2' below the bottom of the I55-56 sign.
7. Cost of bracket and fastening hardware shall be incidental to the I55-56 sign.

SIDE VIEW



ROLLUP SIGN BRACKET
I55-56B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/26/16 PLATE NO. I55-56B.2

PROJECT NO:

HWY:

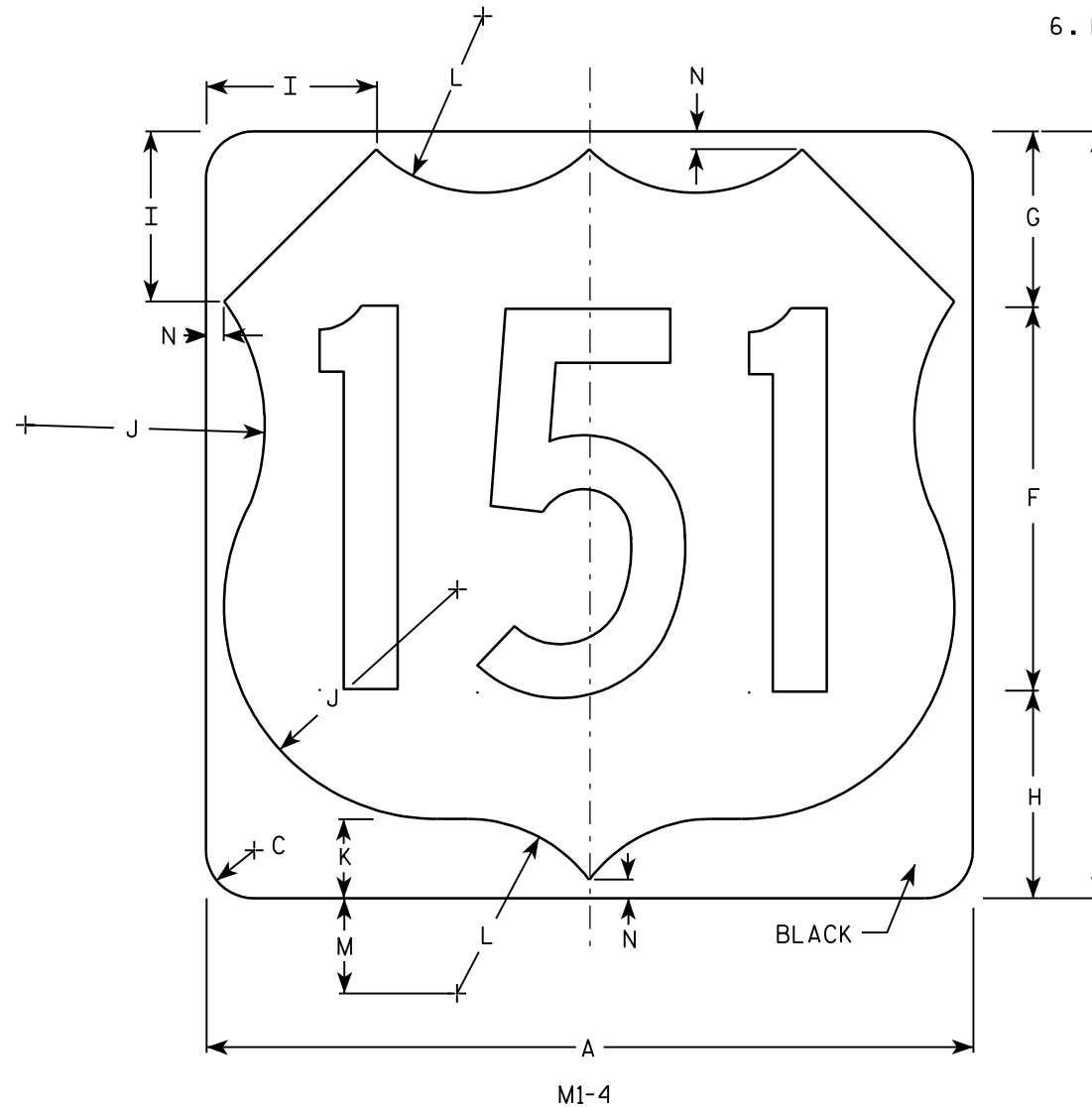
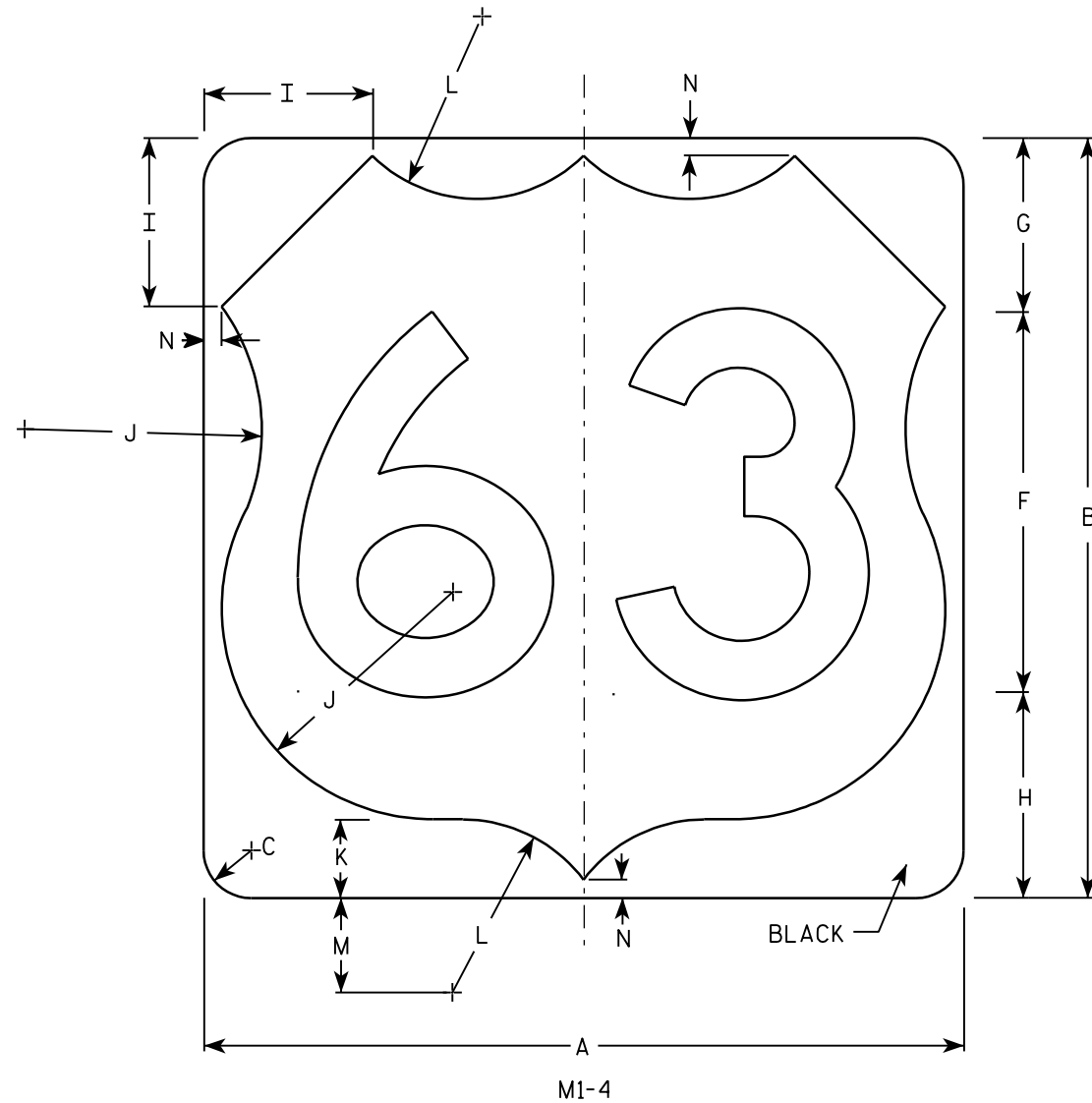
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - See Note 6 - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 6
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
5. Substitute appropriate numerals and adjust
spacing as per Plate A10-1.
6. Permanent Signs
Background - Type H Reflective
Detour or other temporary signs
Background - Reflective



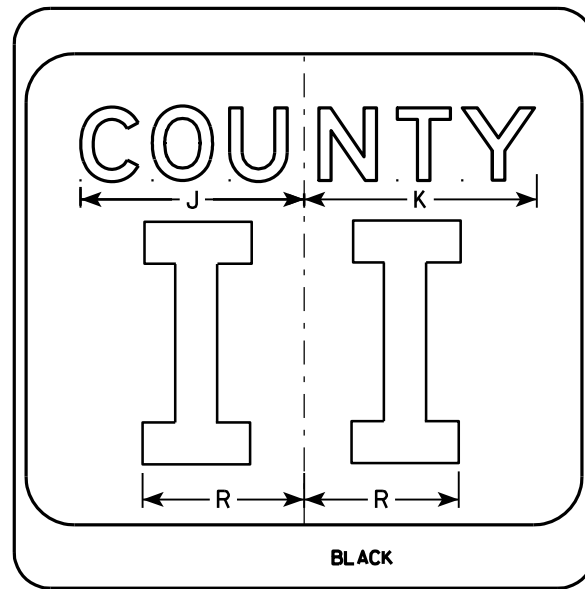
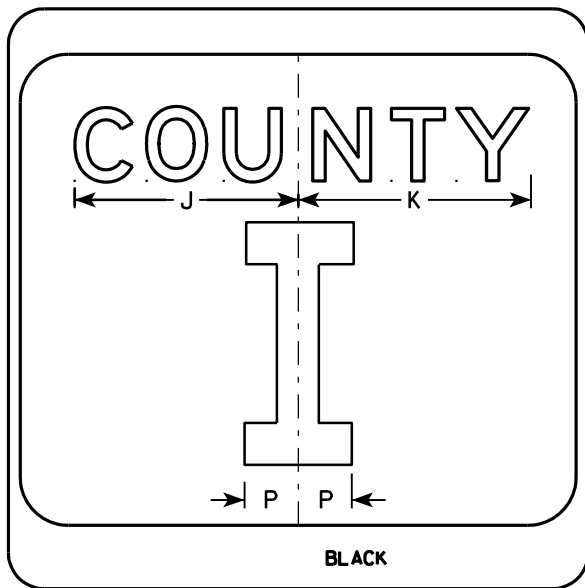
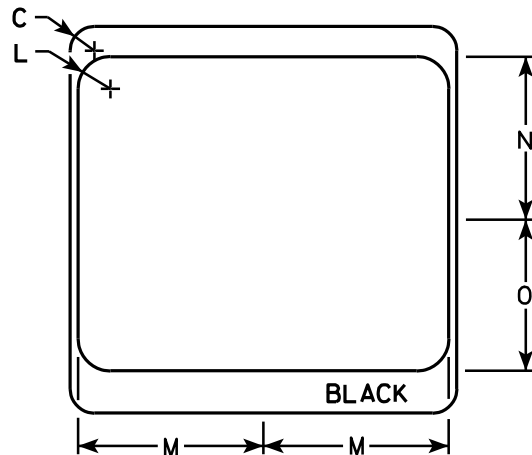
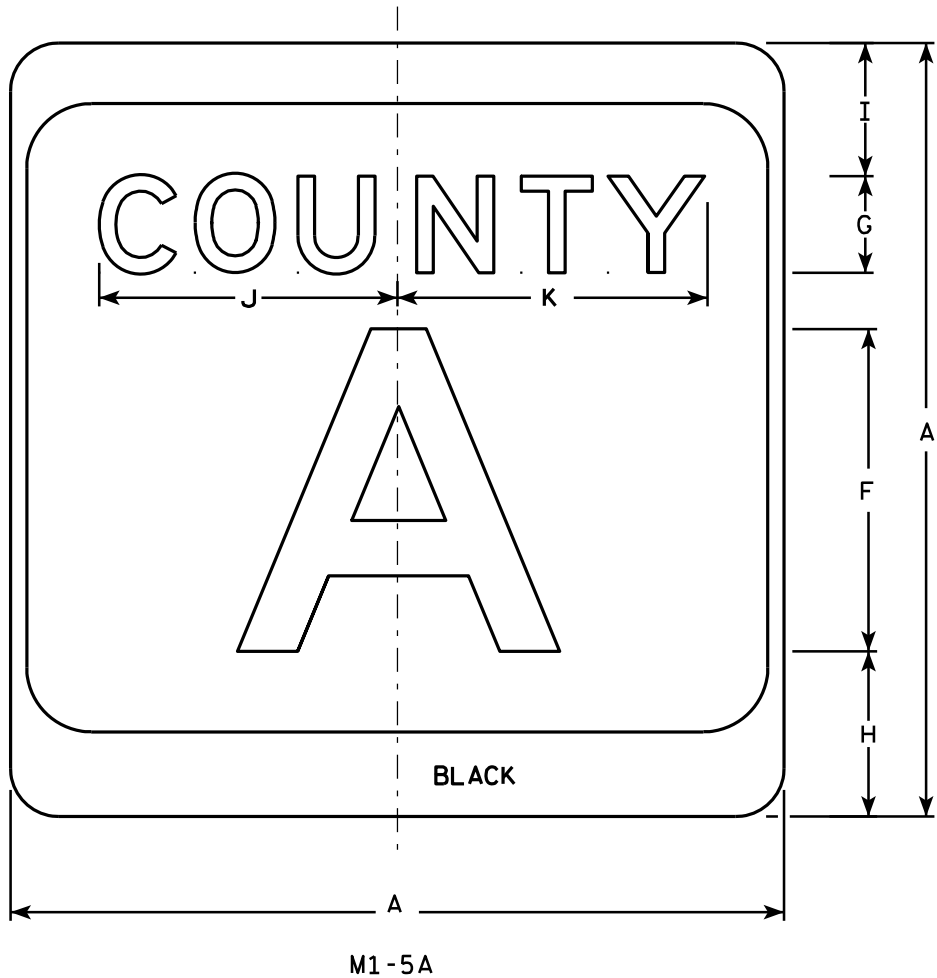
Metric equivalent
for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Areq sq. ft.	Area m ²
1																												
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0	.36
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E
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7



NOTES

- Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - White & Black - See Note 7
Message - Black
- Message Series - see Note 5
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- Message Series E for 1 letter.
Message Series D for 2 letters unless message is too big then Series C.
Message Series C for 3 letters unless message is too big then Series B.
- Substitute appropriate letters & optically center to achieve proper balance.
- Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective

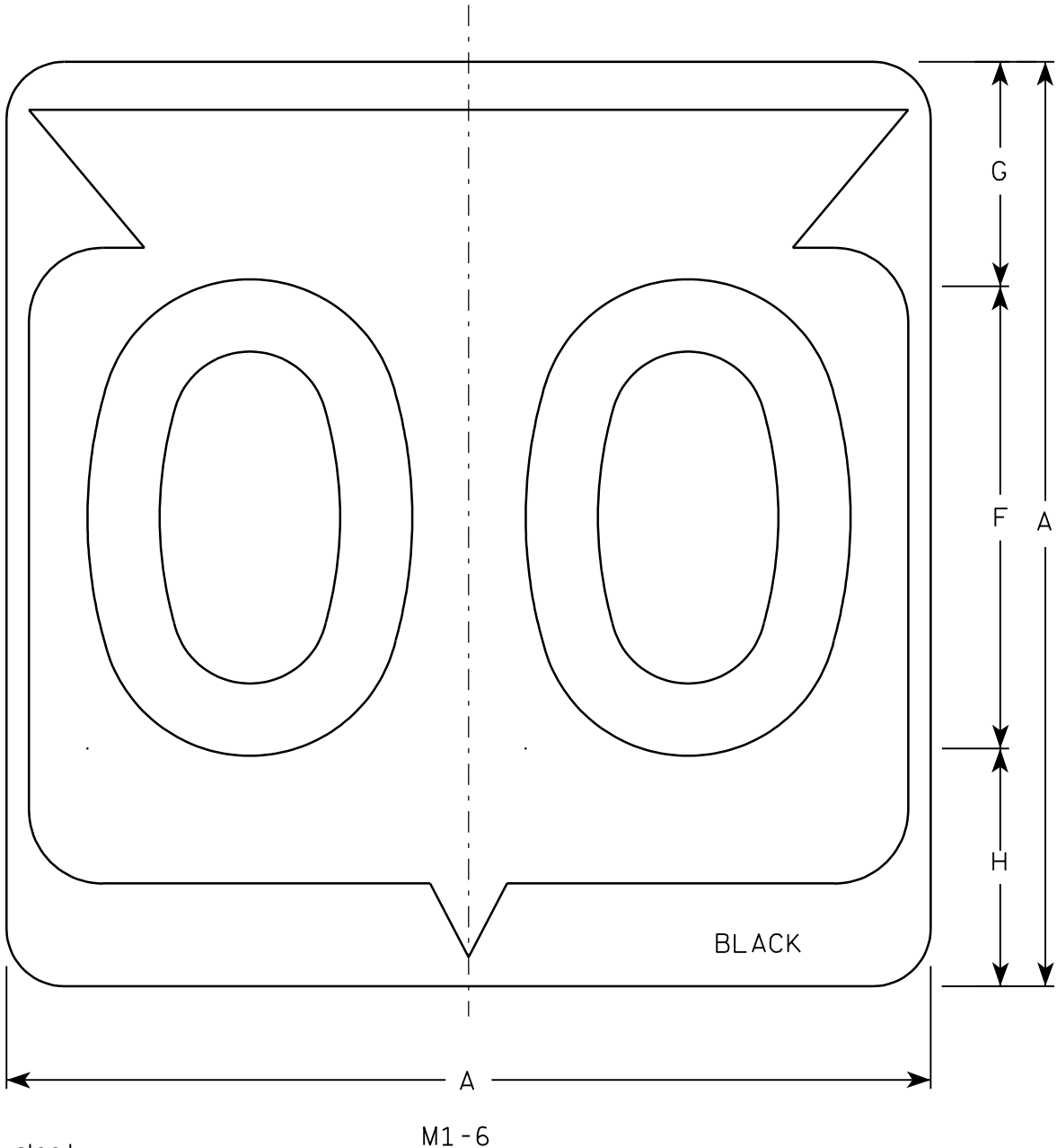
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

CTH MARKER	
M1-5A FOR ASSEMBLIES	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 9/27/11	PLATE NO. M1-5A.8

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
-------------	------	---------	-----------	---

7

7



Metric equivalent
for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

FILE NAME : C:\Users\Projects\tr_stdp\late\M16.DGN

PLOT DATE : 13-OCT-2005 14:55

PLOT BY : DITJPH

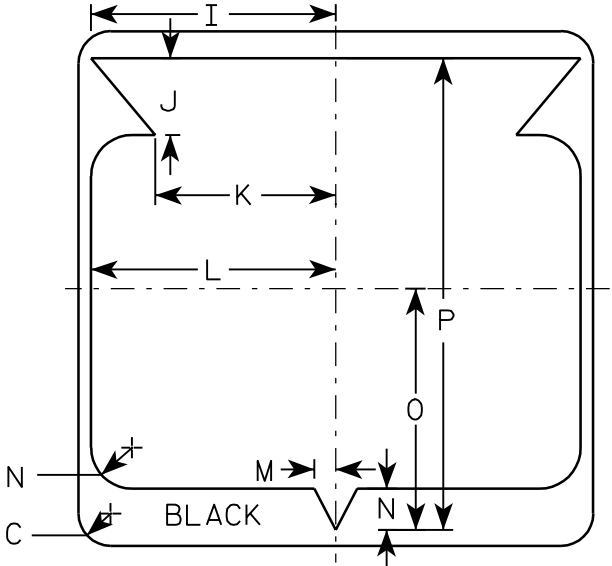
PLOT NAME :

PLOT SCALE : 6.715871:1.000000

WISDOT/CADDS SHEET 42

NOTES

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 6
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
6. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

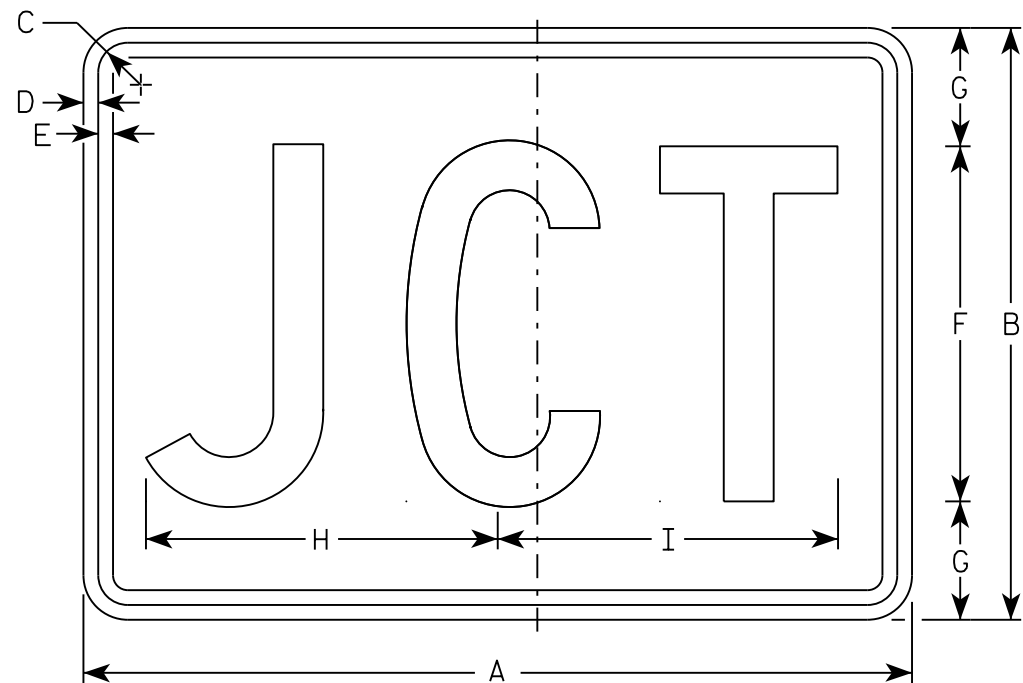
WISCONSIN DEPT OF TRANSPORTATION

APPROVED

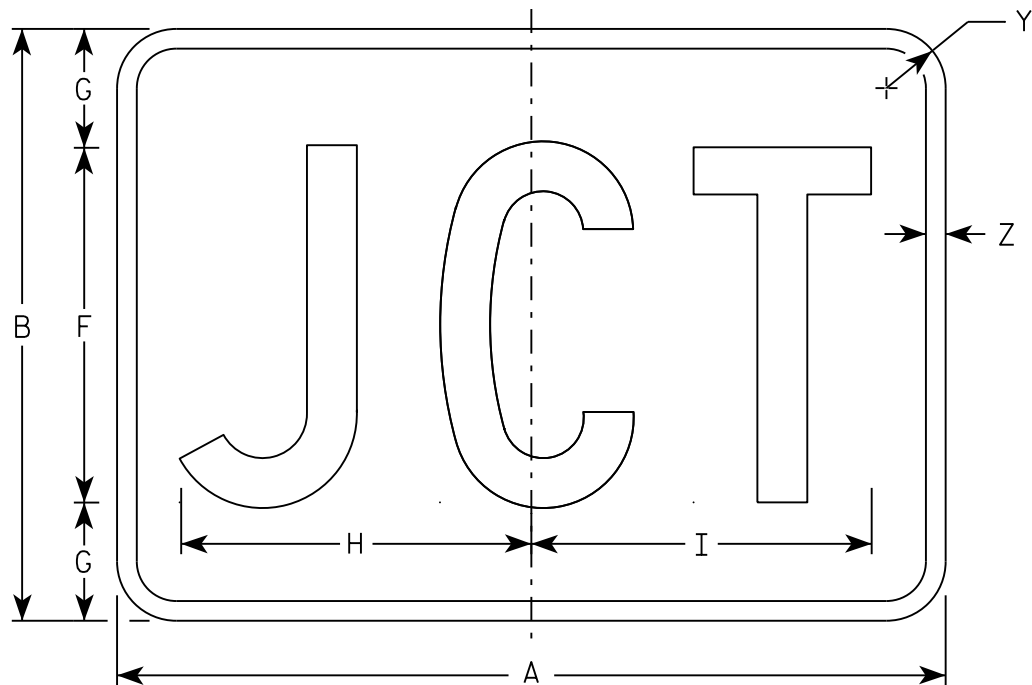
Chester J. Spang
for State Traffic Engineer

DATE 3/20/02

PLATE NO. M1-6.9



M2-1
MM2-1
MP2-1



MB2-1
MK2-1
MN2-1
MR2-1

NOTES

- 1. Sign is Type II - Type H
- 2. Color:
 - Background - See note 5
 - Message - See note 5
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M2-1 Background - White
 Message - Black
 MB2-1 Background - Blue
 Message - White
 MK2-1 Background - Green
 Message - White
 MM2-1 Background - White
 Message - Green
 MN2-1 Background - Brown
 Message - White
 MP2-1 Background - White
 Message - Blue
 MR2-1 Background - Brown
 Message - Yellow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

STANDARD SIGN

M2 - 1

WISCONSIN DEPT OF TRANSPORTATION

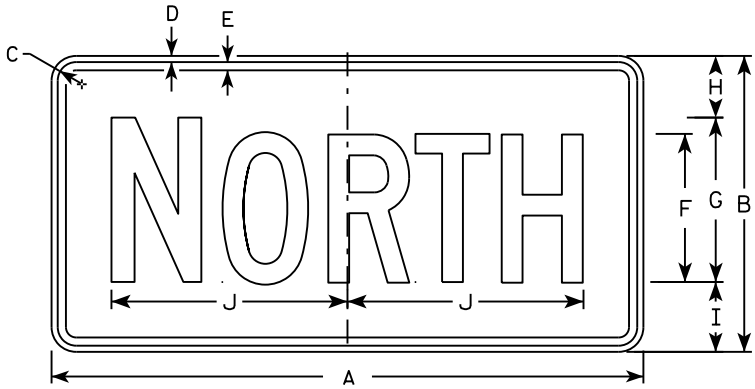
APPROVED

Matthew R. Rauch

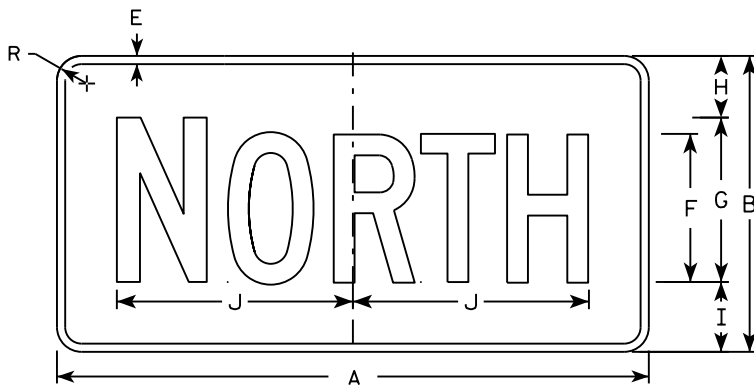
For State Traffic Engineer

DATE 10/15/15

PLATE NO. M2-1.12



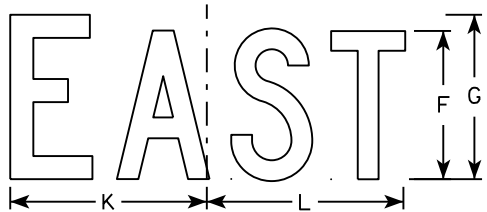
M3-1
MM3-1
MP3-1



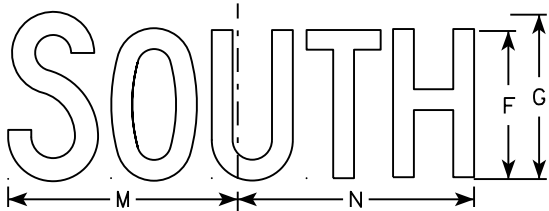
MB3-1
MK3-1
MN3-1



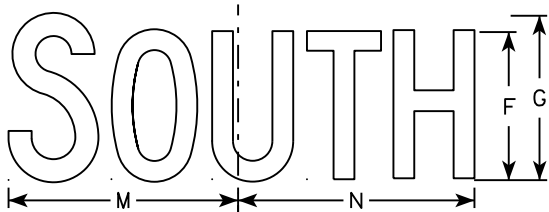
M3-2
MM3-2
MP3-2



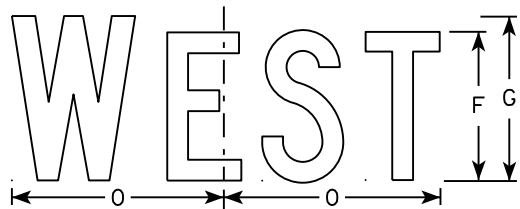
MB3-2
MK3-2
MN3-2



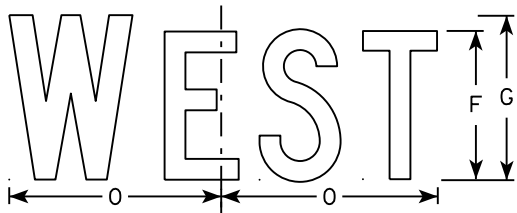
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

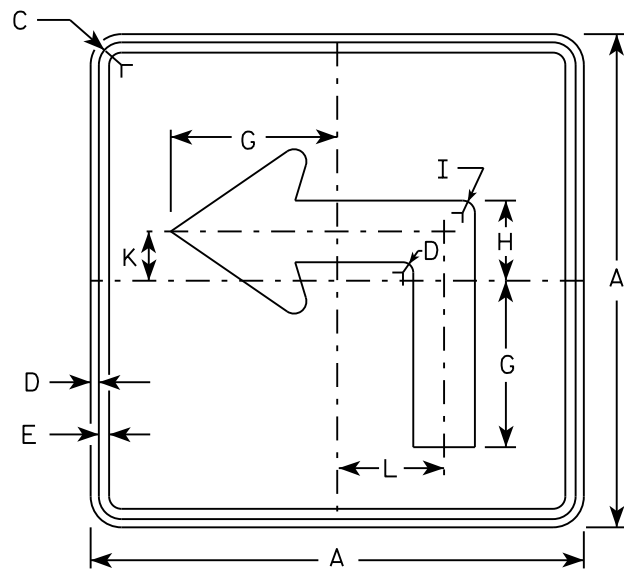
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

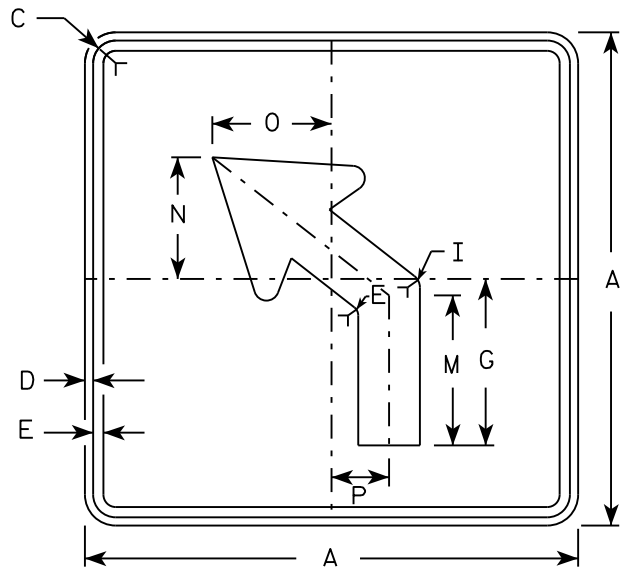
WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

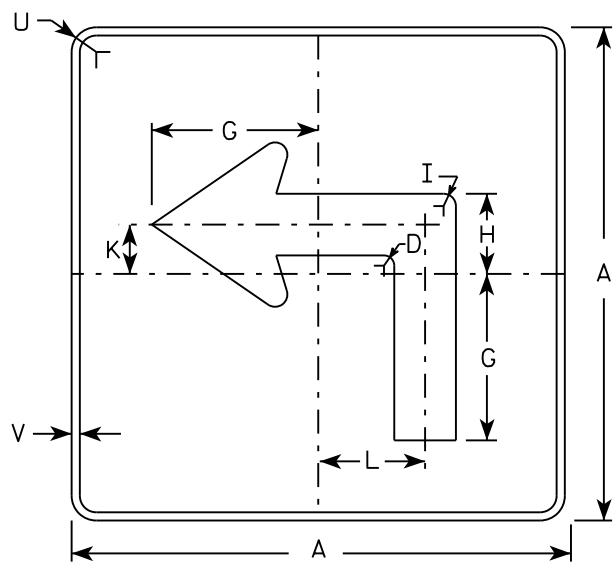
DATE 10/15/15 PLATE NO. M3-1.14



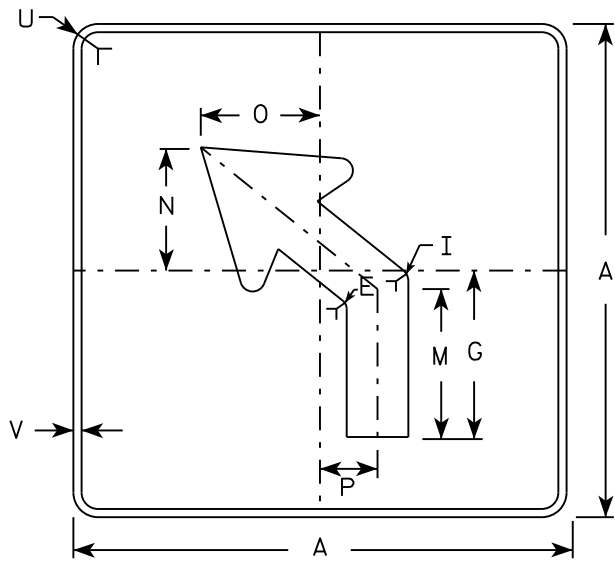
M5-1L
MM5-1L
M05-1L
MP5-1L



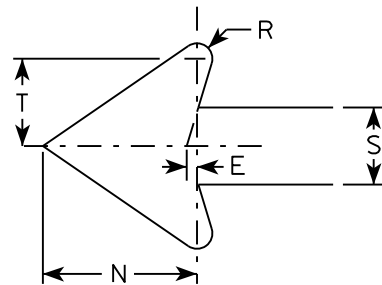
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|---|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White - Type H Reflective |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

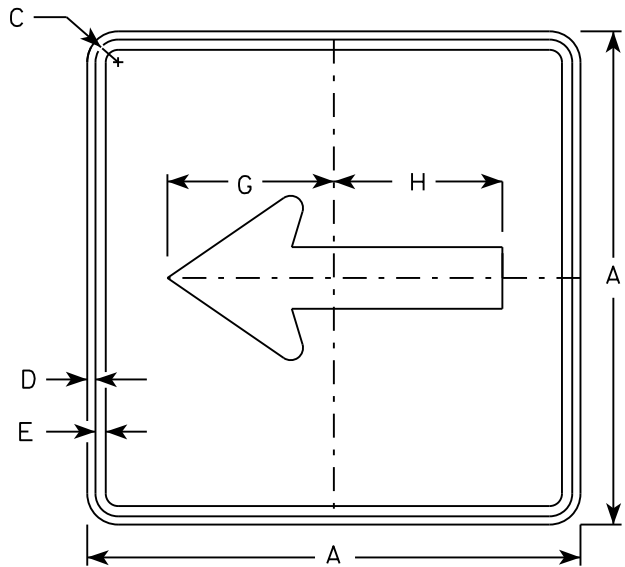
STANDARD SIGN

M5-1 & M5-2

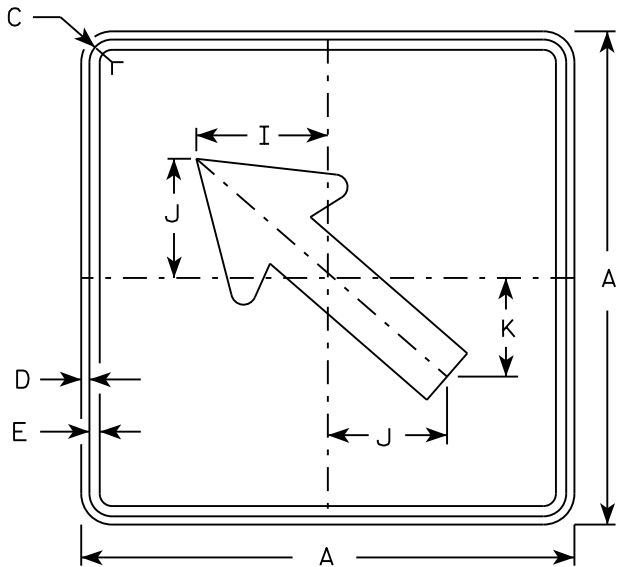
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

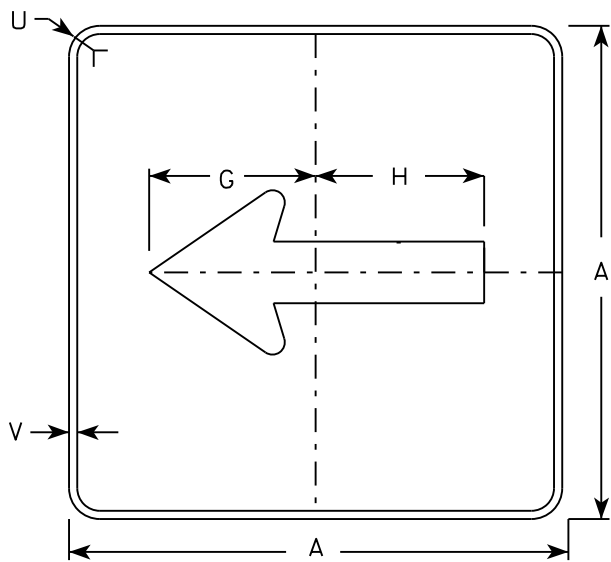
DATE 10/15/15 PLATE NO. M5-1.13



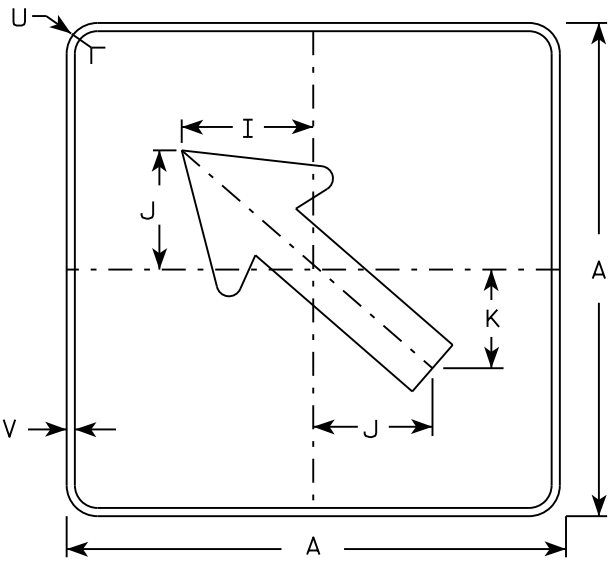
M6 - 1
MM6 - 1
M06 - 1
MP6 - 1



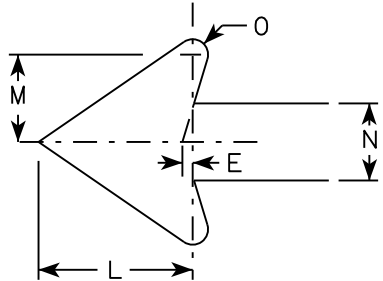
M6 - 2
MM6 - 2
M06 - 2
MP6 - 2



MB6 - 1
MK6 - 1
MN6 - 1
MR6 - 1



MB6 - 2
MK6 - 2
MN6 - 2
MR6 - 2



NOTES

- 1. Signs are Type II - Type H except as Shown
- 2. Color:
Background - See note 4
Message - See note 4
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

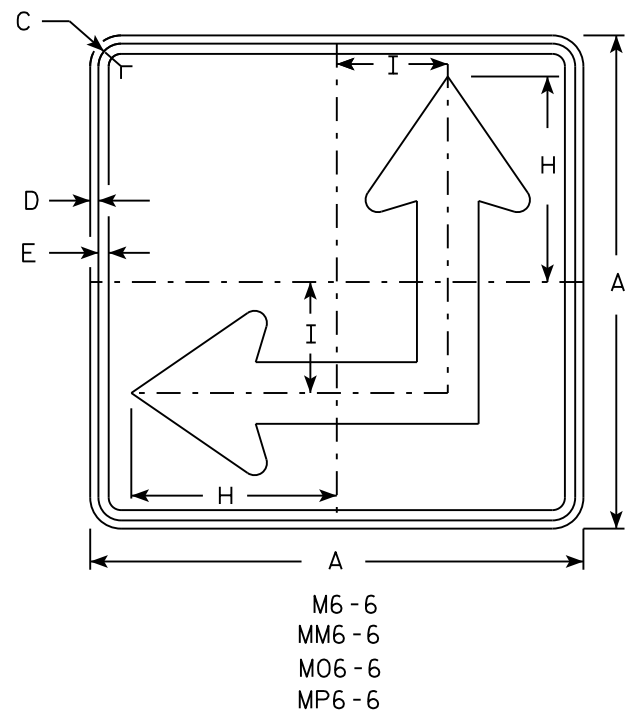
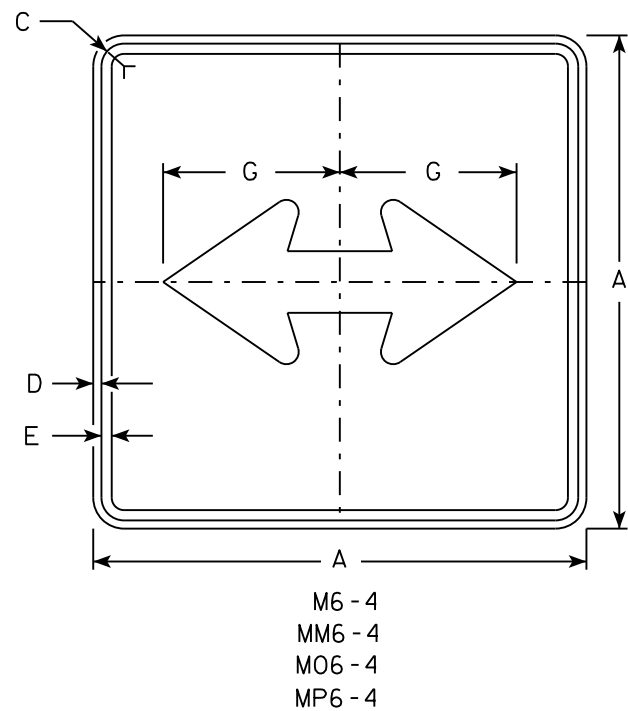
E

STANDARD SIGN
M6 - 1 & M6 - 2
SERIES

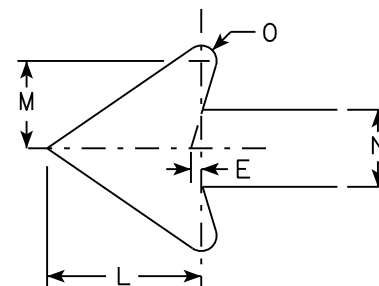
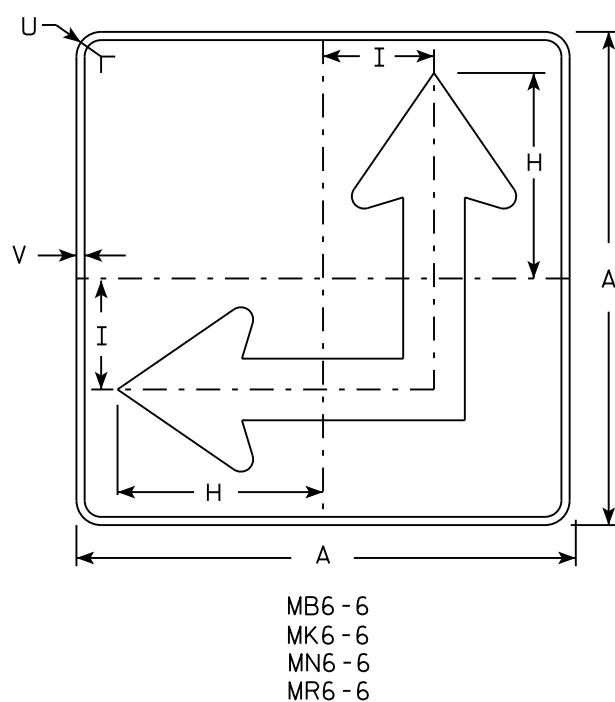
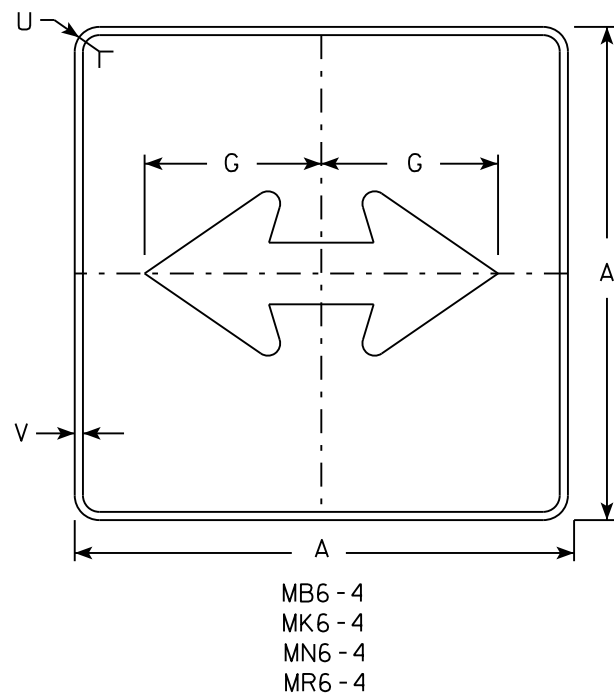
WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15



- NOTES
- Signs are Type II - Type H except as Shown
 - Color:
Background - See Note 4
Message - See Note 4
 - Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
 - M6-4 and M6-6 Background - White
Message - Black
MB6-4 and MB6-6 Background - Blue
Message - White
MK6-4 and MK6-6 Background - Green
Message - White
MM6-4 and MM6-6 Background - White
Message - Green
MN6-4 and MN6-6 Background - Brown
Message - White
M06-4 and M06-6 Background - Orange - Type F Reflective
Message - Black
MP6-4 and MP6-6 Background - White
Message - Blue
MR6-4 and MR6-6 Background - Brown
Message - Yellow
 - M6-6R same as M6-6L except arrow points ahead and right.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	8 3/4	4 1/4			5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

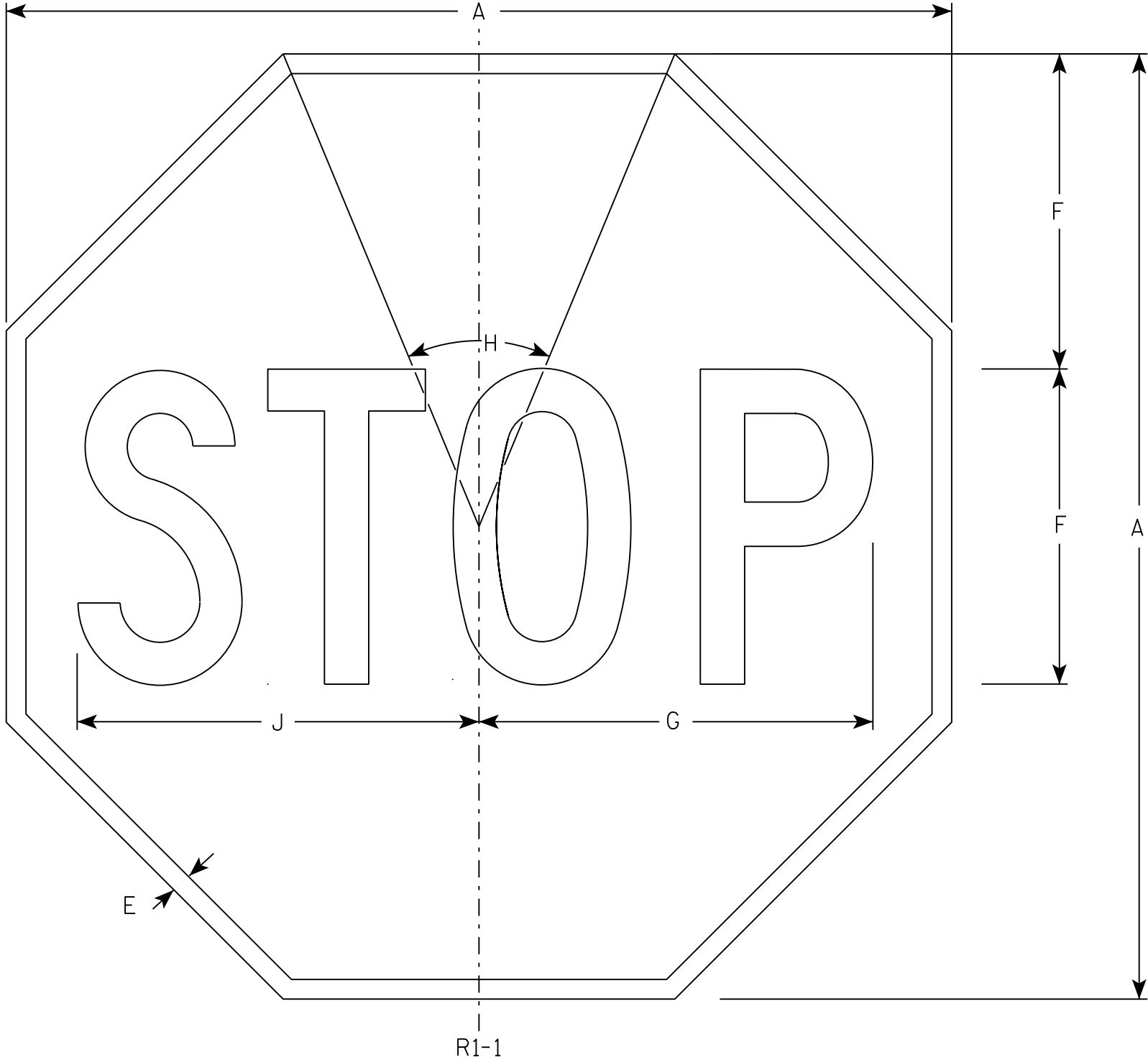
PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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STANDARD SIGN
M6 - 4 & M6 - 6
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-4.10



NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
 - Background - Red
 - Message - White
- 3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN

R1-1

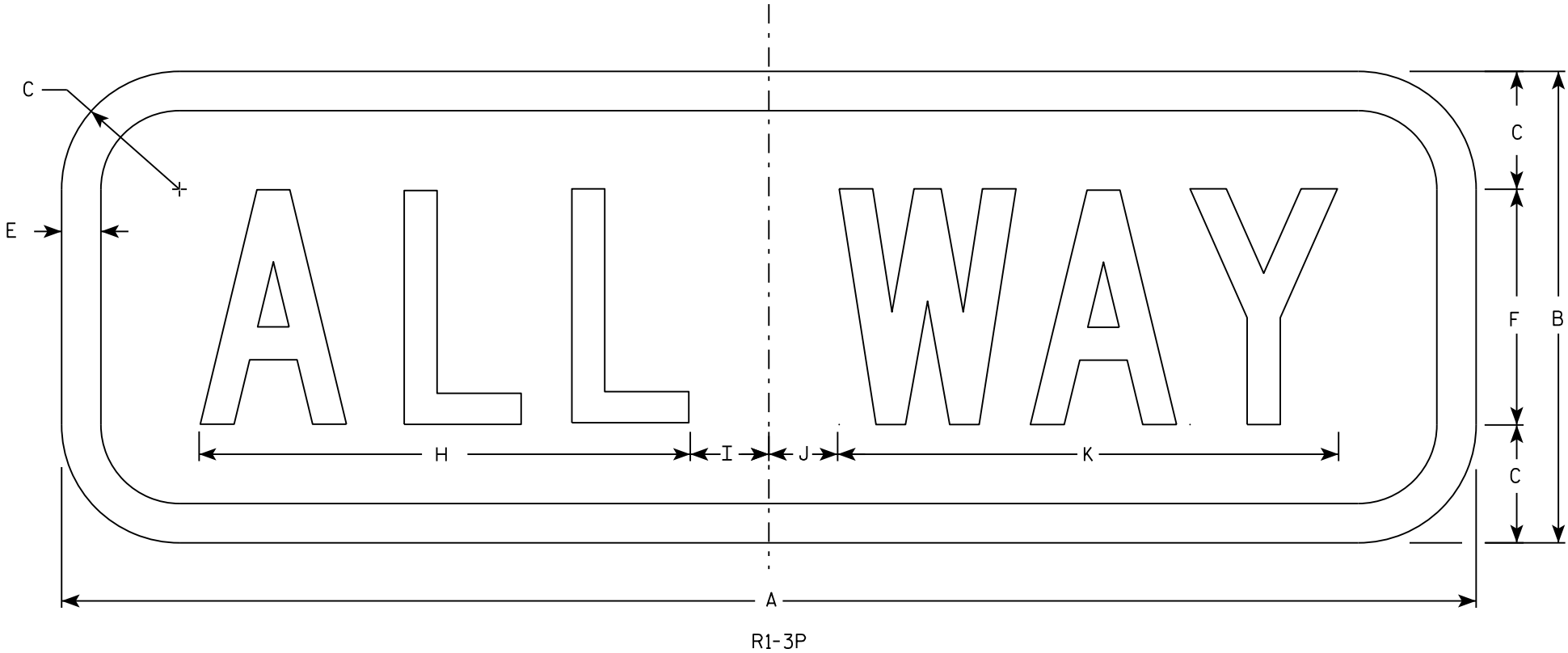
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

NOTES

- 1. Sign is Type II - Type H Reflective
- 2. Color:
Background - Red
Message - White
- 3. Message Series - C
- 4. For 30"x30" R1-1 use 18"x6" R1-3P sign
For 36"x36" R1-1 use 24"x9" R1-3P sign
For 48"x48" R1-1 use 30"x12" R1-3P sign



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																0.75
2S	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																1.5
2M	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
3	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
4	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5
5	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

STANDARD SIGN

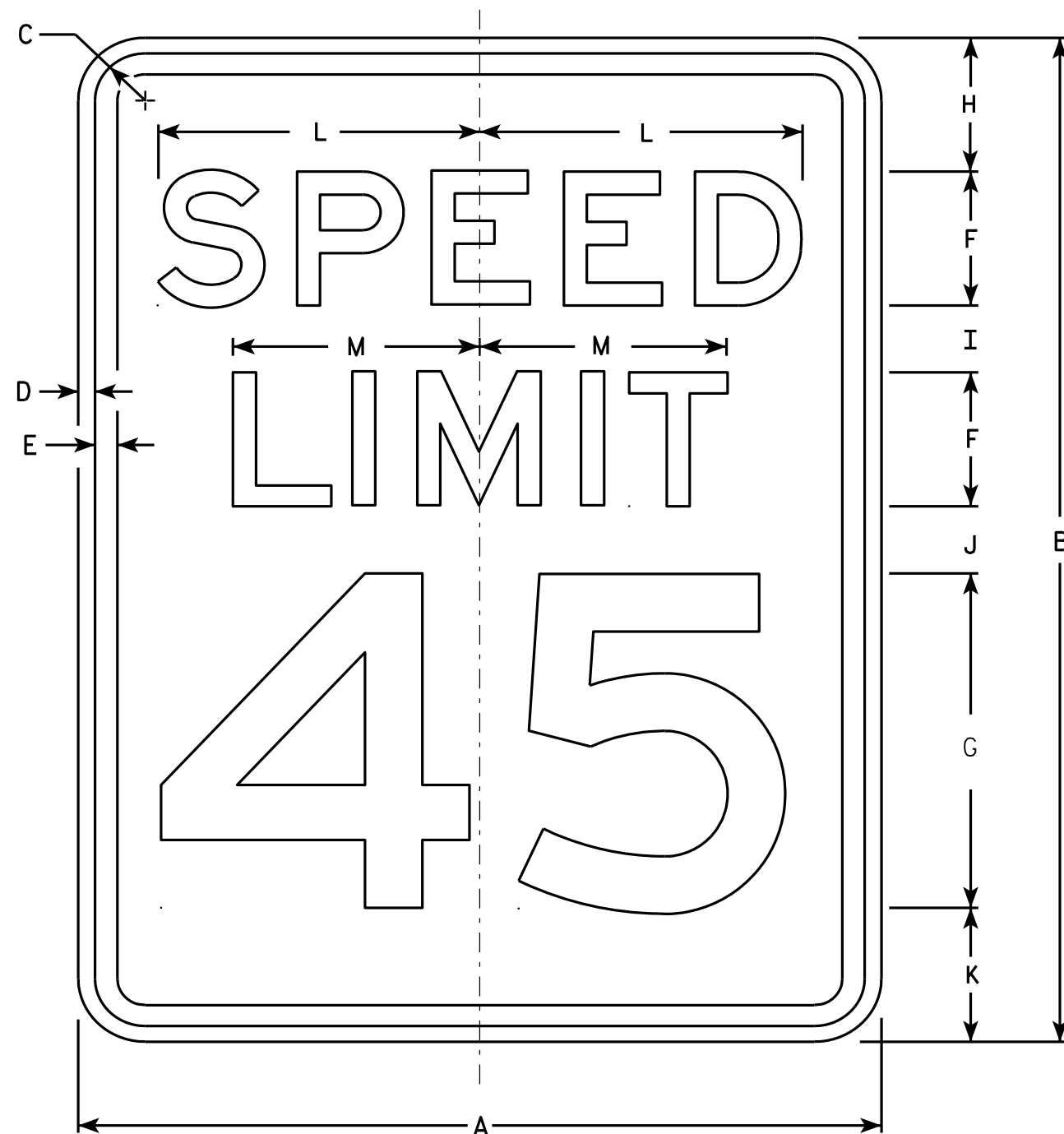
R1 - 3P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*

for State Traffic Engineer

DATE 11/29/16 PLATE NO. R1-3P.3



R2-1

NOTES

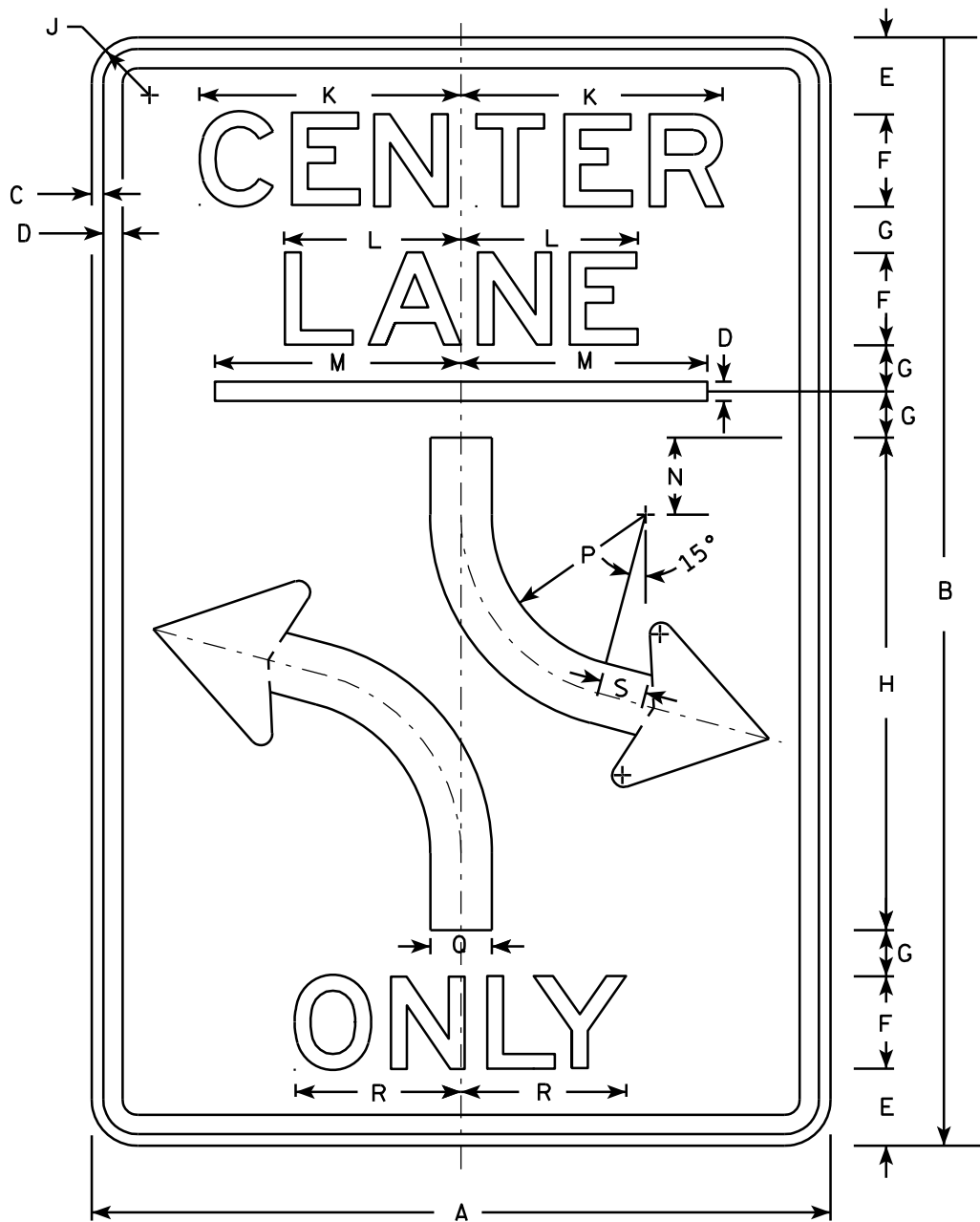
1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 5/26/10 PLATE NO. R2-1.13

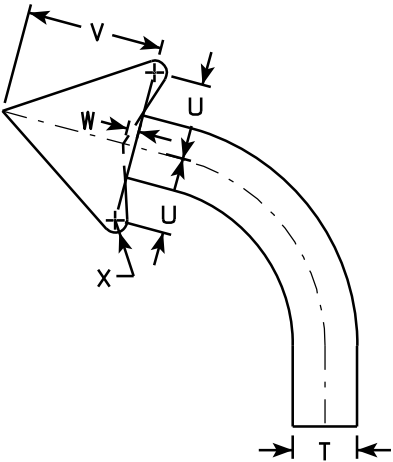
PROJECT NO: HWY: COUNTY: SHEET NO: E



R3-9B

NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - White
Message - Black
- 3. Message Series - E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	$\frac{3}{8}$	$\frac{1}{2}$	2 $\frac{1}{2}$	3	1 $\frac{1}{2}$	16		1 $\frac{1}{2}$	8 $\frac{1}{2}$	5 $\frac{3}{4}$	8	2 $\frac{1}{2}$		6	2	5 $\frac{1}{8}$	1 $\frac{1}{2}$		2 $\frac{3}{8}$	4 $\frac{3}{8}$	$\frac{3}{8}$				6.0
2M	24	36	$\frac{3}{8}$	$\frac{1}{2}$	2 $\frac{1}{2}$	3	1 $\frac{1}{2}$	16		1 $\frac{1}{2}$	8 $\frac{1}{2}$	5 $\frac{3}{4}$	8	2 $\frac{1}{2}$		6	2	5 $\frac{1}{8}$	1 $\frac{1}{2}$		2 $\frac{3}{8}$	4 $\frac{3}{8}$	$\frac{3}{8}$				6.0
3	36	48	$\frac{5}{8}$	$\frac{7}{8}$	3 $\frac{1}{2}$	5	1 $\frac{1}{2}$	20		2 $\frac{1}{4}$	14 $\frac{1}{8}$	9 $\frac{1}{2}$	12	3		4	3	9 $\frac{7}{8}$	2		3 $\frac{1}{2}$	6 $\frac{1}{8}$	$\frac{1}{2}$				12.0
4																											
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

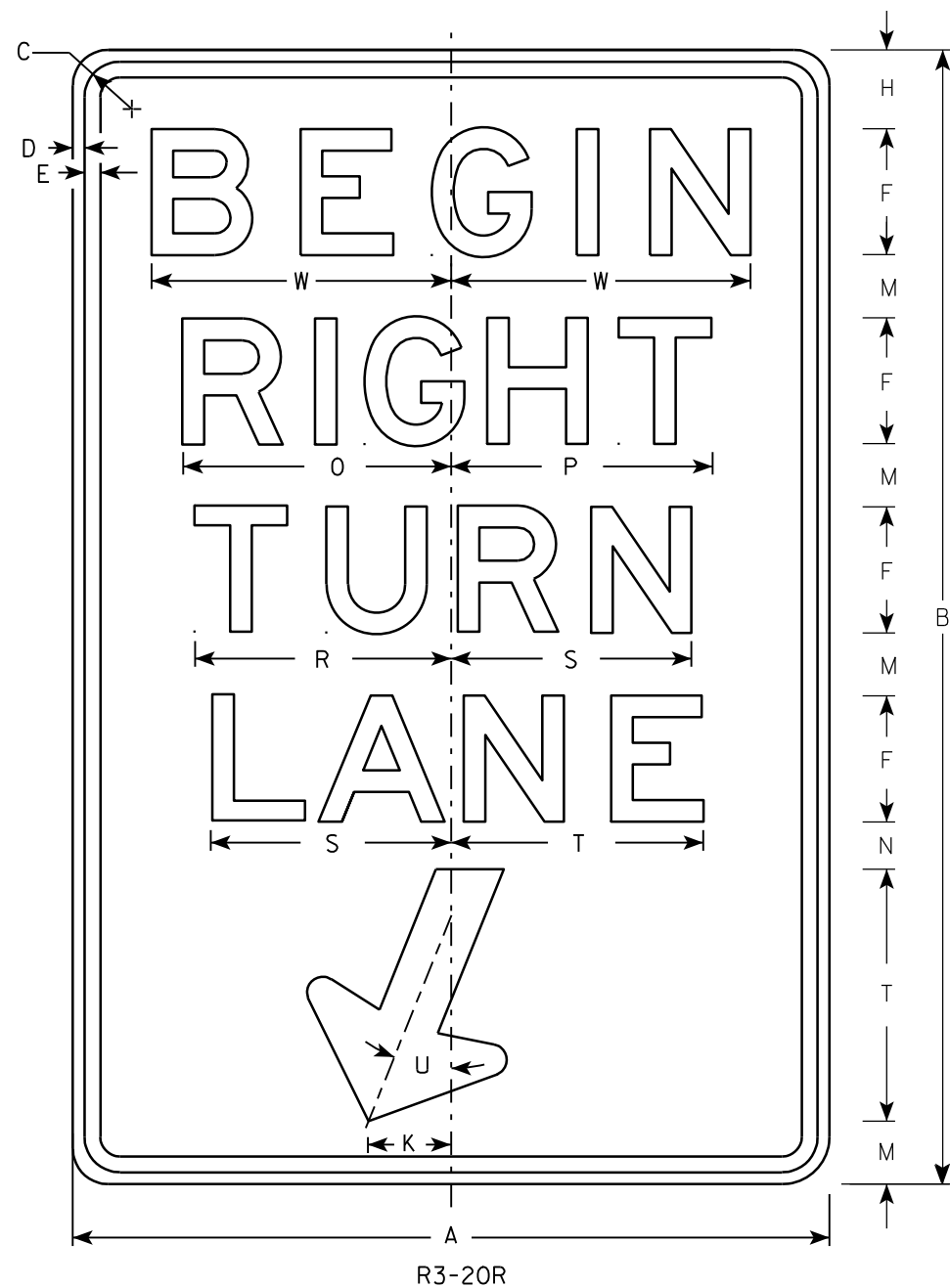
E

STANDARD SIGN
R3-9B

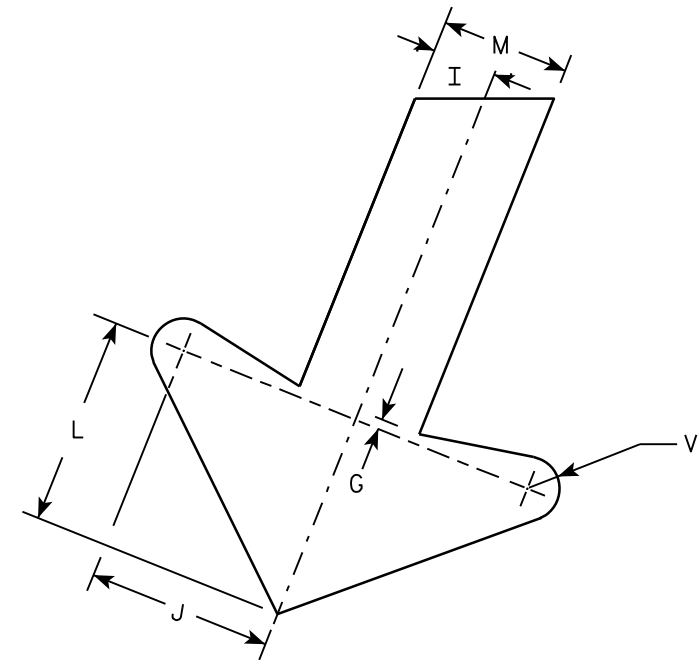
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/24/2011 PLATE NO. R3-9B.5



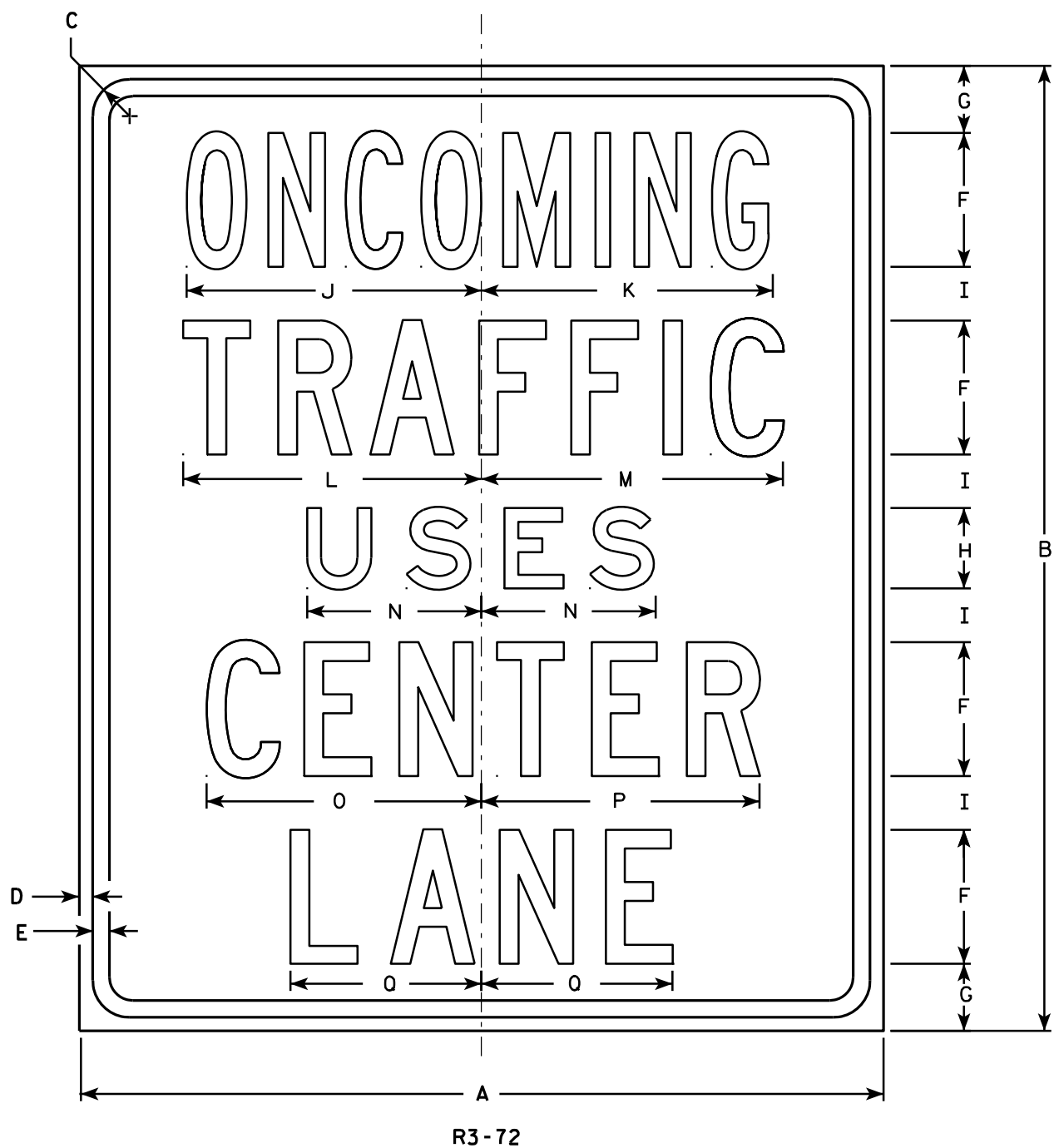
- NOTES
1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
 2. Color:
Background - White
Message - Black
 3. Message Series - E
 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
2M	24	36	1 1/8	3/8	1/2	4	1/4	2 1/2	1	2 7/8	2 5/8	3 1/4	2	1 1/2	8 1/2	8 1/4		8 1/8	7 5/8	8	22°	1/2	9 1/2				6.0
3	36	54	1 3/4	1/2	5/8	6	3/8	3 3/4	1 1/2	4 1/4	4	4 7/8	3	2 1/4	12 3/4	12 1/2		12 1/4	11 1/2	12	22°	3/4	13 1/4				13.5
4																											
5																											

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - Line 1 is Series B, Lines 2, 4 and 5 are Series C, and Line 3 is Series E.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	36	1 3/8	1/2	5/8	5	2 1/2	3	2	11	10 7/8	11 1/8	11 1/4	6 1/2	10 1/4	10 3/8	7 1/8										7.50
2S	30	36	1 3/8	1/2	5/8	5	2 1/2	3	2	11	10 7/8	11 1/8	11 1/4	6 1/2	10 1/4	10 3/8	7 1/8										7.50
2M	30	36	1 3/8	1/2	5/8	5	2 1/2	3	2	11	10 7/8	11 1/8	11 1/4	6 1/2	10 1/4	10 3/8	7 1/8										7.50
3																											
4																											
5																											

STANDARD SIGN R3-72

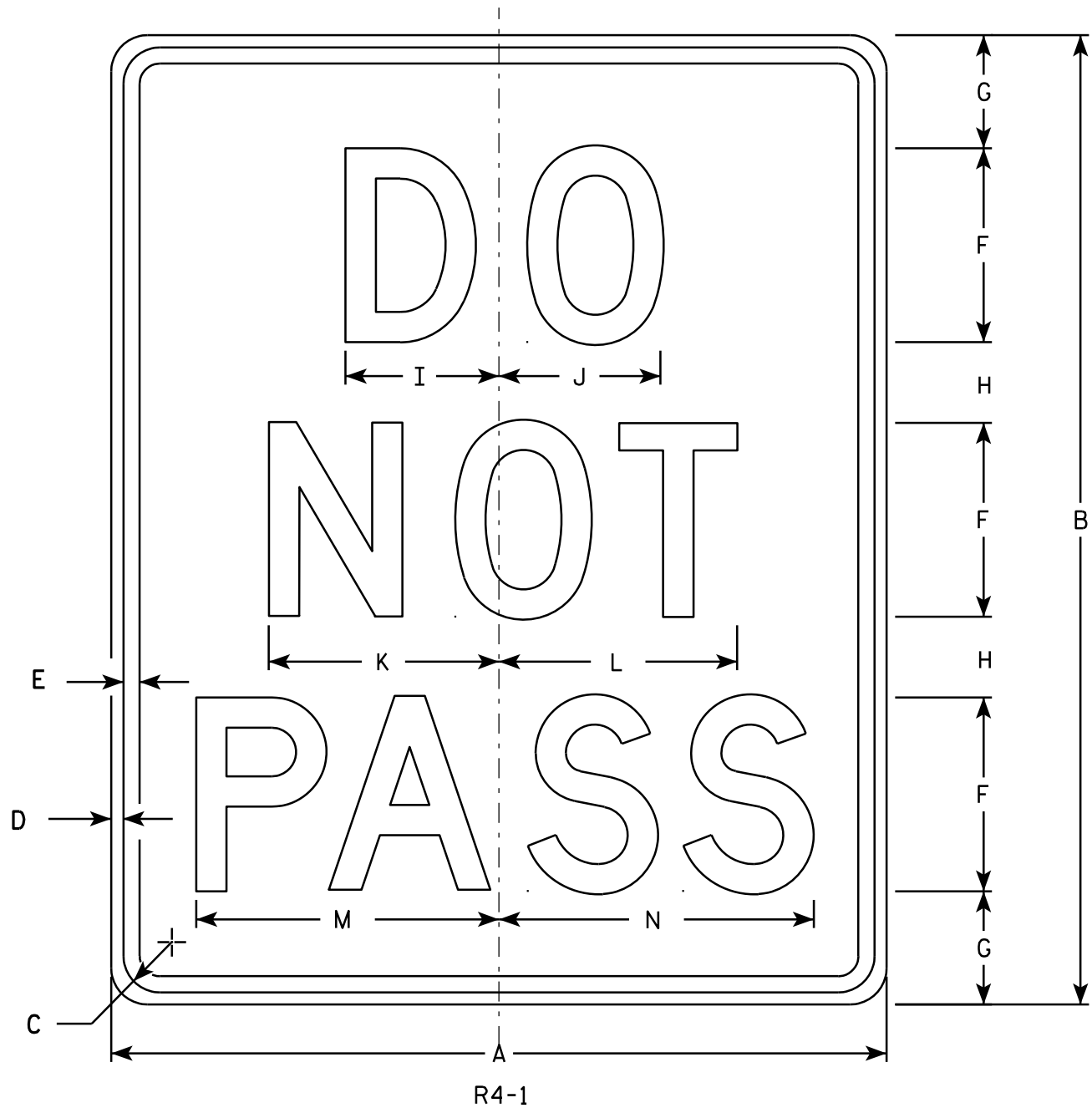
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R3-72.5

PROJECT NO: HWY: COUNTY: SHEET NO: E

7



NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
 - Background - White
 - Message - Black
- 3. Message Series - D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

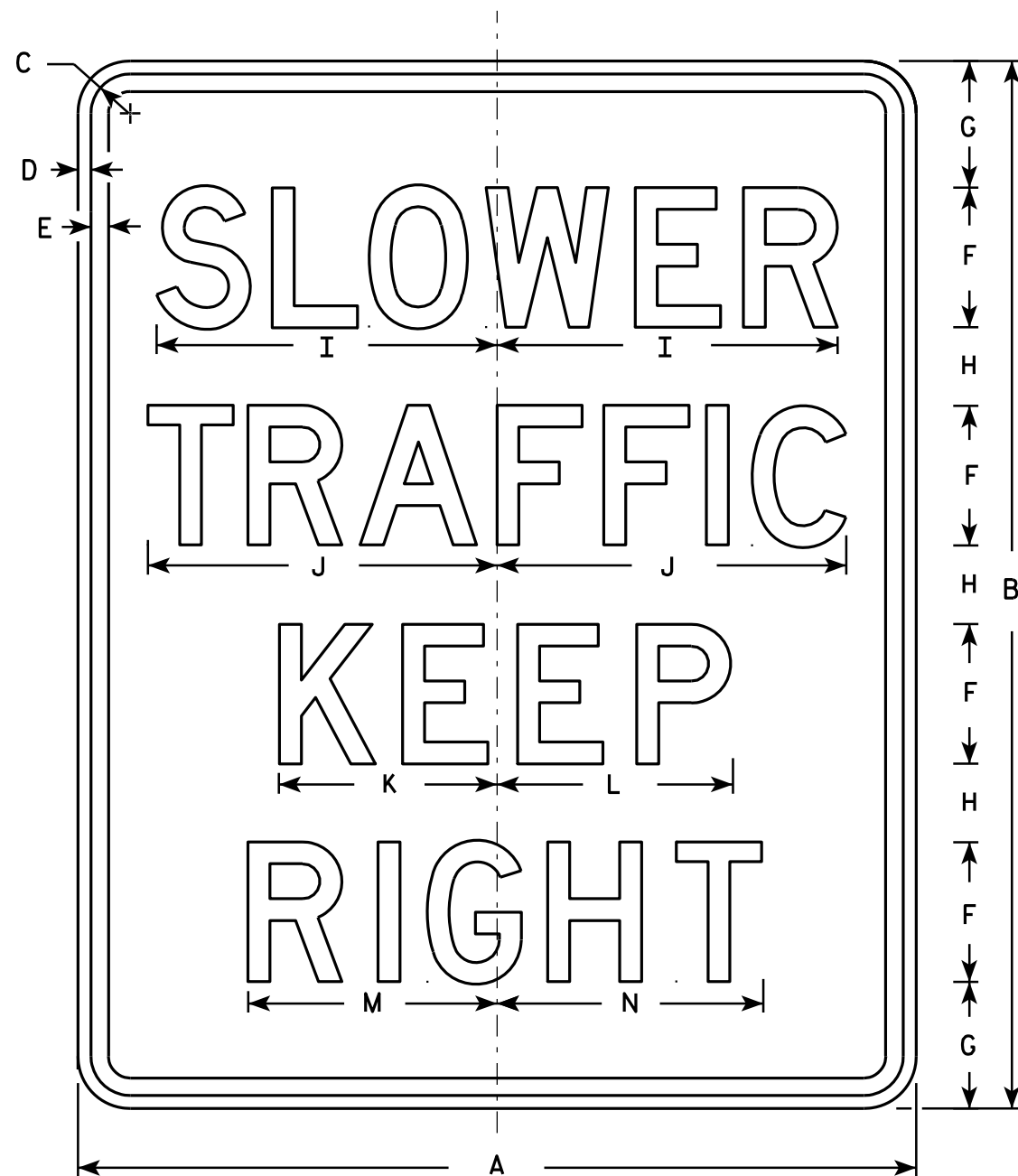
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	4	3 1/2	2 1/2	3 1/8	3 1/4	4 3/4	4 7/8	6 1/4	6 1/2													3.0
2S	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
2M	24	30	1 1/8	3/8	1/2	6	3 1/2	2 1/2	4 3/4	5	7 1/8	7 3/8	9 3/8	9 3/4													5.0
3																											
4	36	48	1 5/8	5/8	3/4	8	7	5	6 1/4	6 5/8	9 1/2	9 3/4	12 1/2	13													12.0
5	48	60	2 1/4	3/4	1	10	8	7	7 3/4	8 3/8	11 7/8	12 1/4	15 5/8	16 1/4													20.0

STANDARD SIGN
R4 - 1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-1.7



R4-3

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	30	1 1/8	3/8	1/2	4	3 5/8	2 1/4	9 3/4	10	6 1/4	6 3/4	7 1/8	7 5/8													5.0
2M	24	30	1 1/8	3/8	1/2	4	3 5/8	2 1/4	9 3/4	10	6 1/4	6 3/4	7 1/8	7 5/8													5.0
3	36	48	1 5/8	5/8	3/4	6	6	4	14 5/8	15	9 3/8	10	10 3/4	11 3/8													12.0
4	36	48	1 5/8	5/8	3/4	6	6	4	14 5/8	15	9 3/8	10	10 3/4	11 3/8													12.0
5	48	60	2 1/4	3/4	1	8	7 1/4	4 1/2	19 1/2	20	12 1/2	13 1/2	14 1/4	15 1/4													20.0

STANDARD SIGN R4-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-3.8

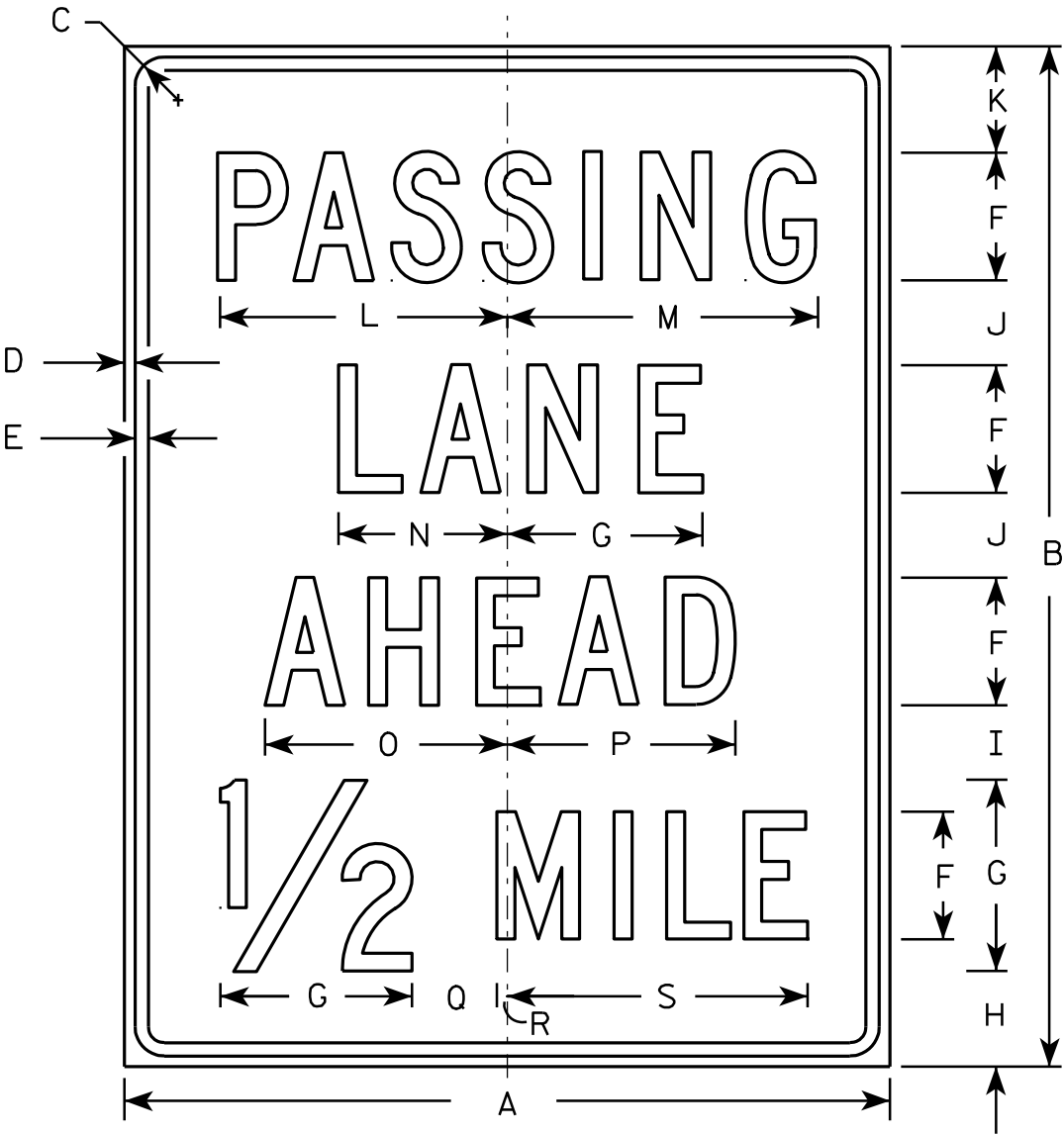
PROJECT NO:

SHEET NO:

E

NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - White
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R4-51

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	48	1 ³ / ₈	1/2	⁵ / ₈	6	9	4 1/2	3 1/2	4	5	13 ⁵ / ₈	14 1/2	8	11 ³ / ₈	10 ³ / ₄	4	1/2	14 1/8								12.0
2M	36	48	1 ³ / ₈	1/2	⁵ / ₈	6	9	4 1/2	3 1/2	4	5	13 ⁵ / ₈	14 1/2	8	11 ³ / ₈	10 ³ / ₄	4	1/2	14 1/8								12.0
3																											
4																											
5																											

STANDARD SIGN
R4-51

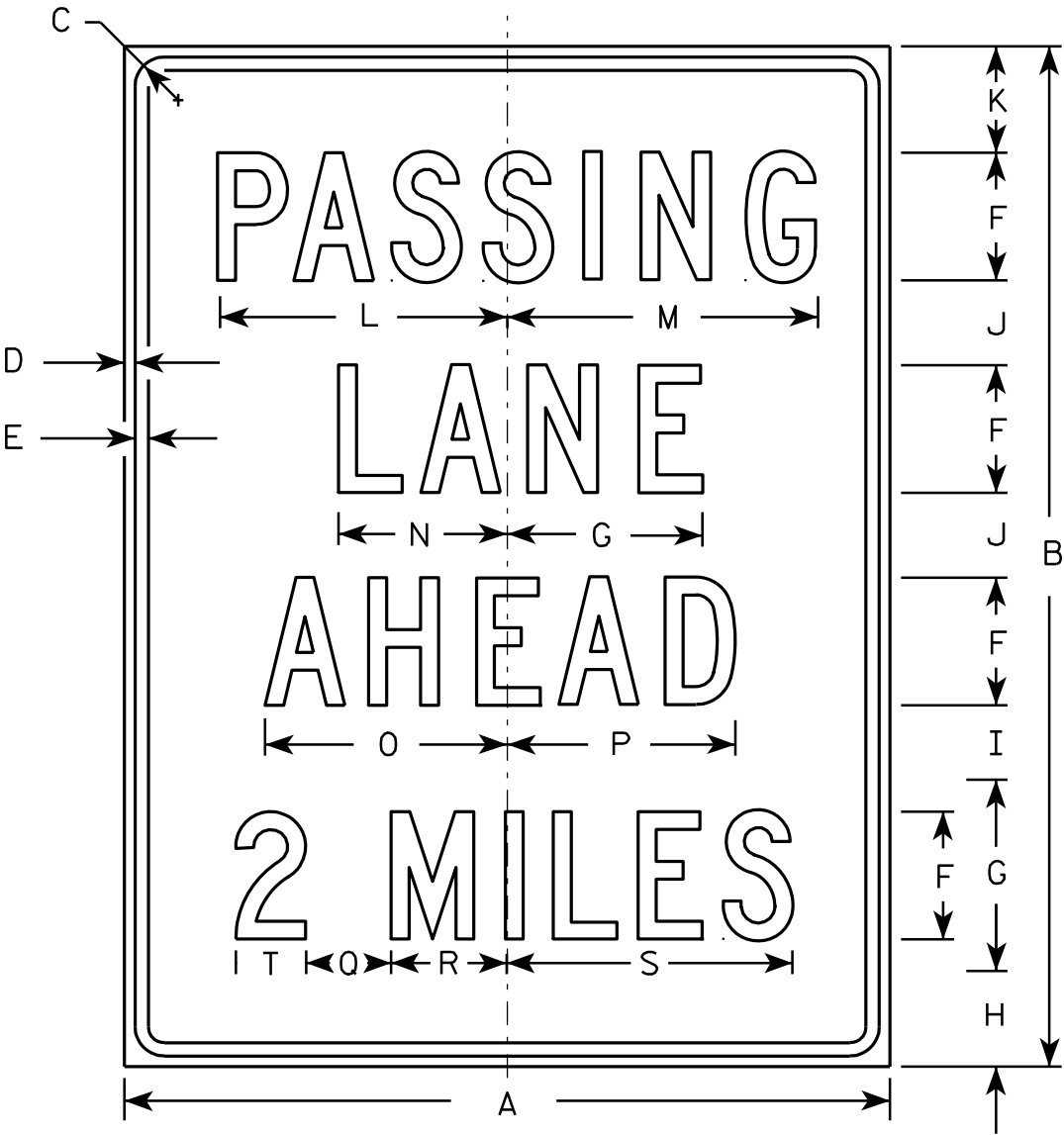
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/29/2011 PLATE NO. R4-51.4

NOTES

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - White
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R4-51A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	48	1 ³ / ₈	1/2	⁵ / ₈	6	9 ¹ / ₄	4 ¹ / ₂	3 ¹ / ₂	4	5	13 ⁵ / ₈	14 ¹ / ₂	8	11 ³ / ₈	10 ³ / ₄	4	5 ¹ / ₂	13 ³ / ₈	3 ¹ / ₄							12.0
2M	36	48	1 ³ / ₈	1/2	⁵ / ₈	6	9 ¹ / ₄	4 ¹ / ₂	3 ¹ / ₂	4	5	13 ⁵ / ₈	14 ¹ / ₂	8	11 ³ / ₈	10 ³ / ₄	4	5 ¹ / ₂	13 ³ / ₈	3 ¹ / ₄							12.0
3																											
4																											
5																											

STANDARD SIGN
R4-51A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/29/2011 PLATE NO. R4-51A.2

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

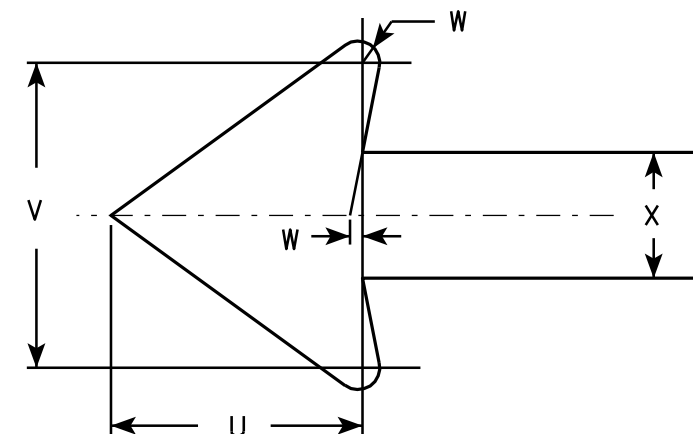
E



R7-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)
R7-1L (left arrow)
R7-1R (right arrow)



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

PROJECT NO: HWY: COUNTY: SHEET NO: E



R9-56

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series E.
Lines 2, 3, 4, 5 and 6 are Series C.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	18	24	1 1/8	3/8	1/2	3	1 1/2	2	1	3 1/8	6 3/8		5 1/4	2 1/8	5 3/4												3.0
2M	18	24	1 1/8	3/8	1/2	3	1 1/2	2	1	3 1/8	6 3/8		5 1/4	2 1/8	5 3/4												3.0
3																											
4																											
5																											

STANDARD SIGN R9-56

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/4/2011 PLATE NO. R9-56.5

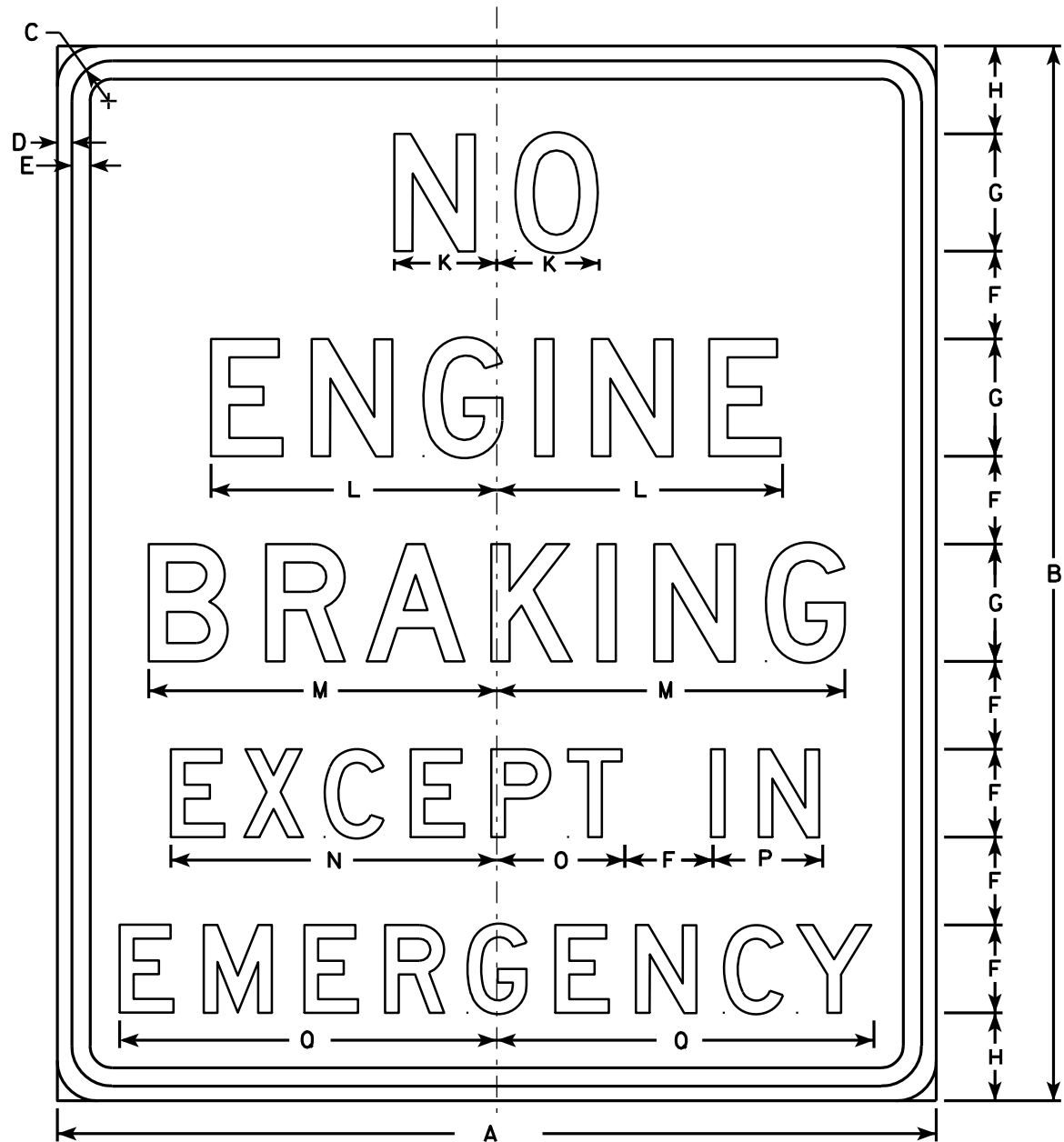
PROJECT NO:

HWY:

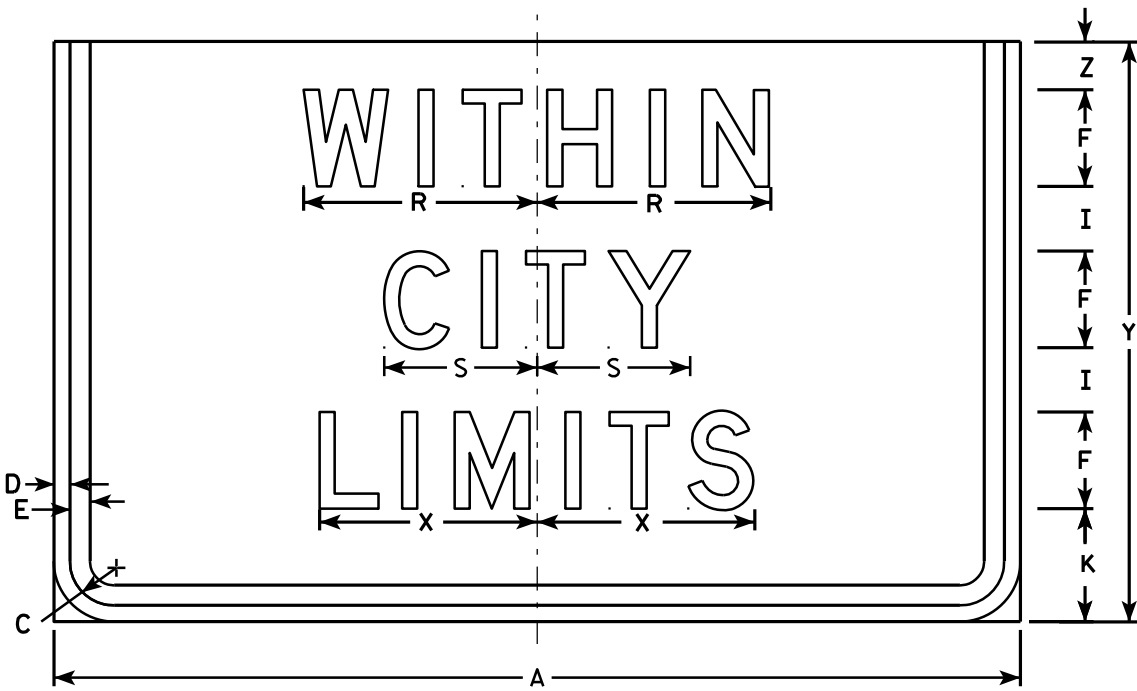
COUNTY:

SHEET NO:

E



R10-64



R10-64F

Area sq. ft.
3.75
7.0

VILLAGE

TOWNSHIP

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	36	1 3⁄8	1⁄2	5⁄8	3	4	3	2		3 1⁄2	9 3⁄4	11 7⁄8	11 1⁄8	4 3⁄8	3 3⁄4	12 7⁄8	7 1⁄4	4 3⁄4		8 1⁄4	8 3⁄4	5⁄8	6 3⁄4	18	1 1⁄2	7.5
2S	30	36	1 3⁄8	1⁄2	5⁄8	3	4	3	2		3 1⁄2	9 3⁄4	11 7⁄8	11 1⁄8	4 3⁄8	3 3⁄4	12 7⁄8	7 1⁄4	4 3⁄4		8 1⁄4	8 3⁄4	5⁄8	6 3⁄4	18	1 1⁄2	7.5
2M	30	36	1 3⁄8	1⁄2	5⁄8	3	4	3	2		3 1⁄2	9 3⁄4	11 7⁄8	11 1⁄8	4 3⁄8	3 3⁄4	12 7⁄8	7 1⁄4	4 3⁄4		8 1⁄4	8 3⁄4	5⁄8	6 3⁄4	18	1 1⁄2	7.5
3	42	48	1 3⁄8	1⁄2	5⁄8	4	5	4 1⁄2	3		4	12 1⁄4	14 7⁄8	14 1⁄8	5 1⁄4	4 3⁄4	16 1⁄4	9	6		11	11 1⁄4	1 1⁄2	8 1⁄2	24	2	14.0
4																											
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

NOTES

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:

Background - White

Message - Black
3. Message Series - D See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 2 of R10-64F is Series C for "TOWNSHIP" Only

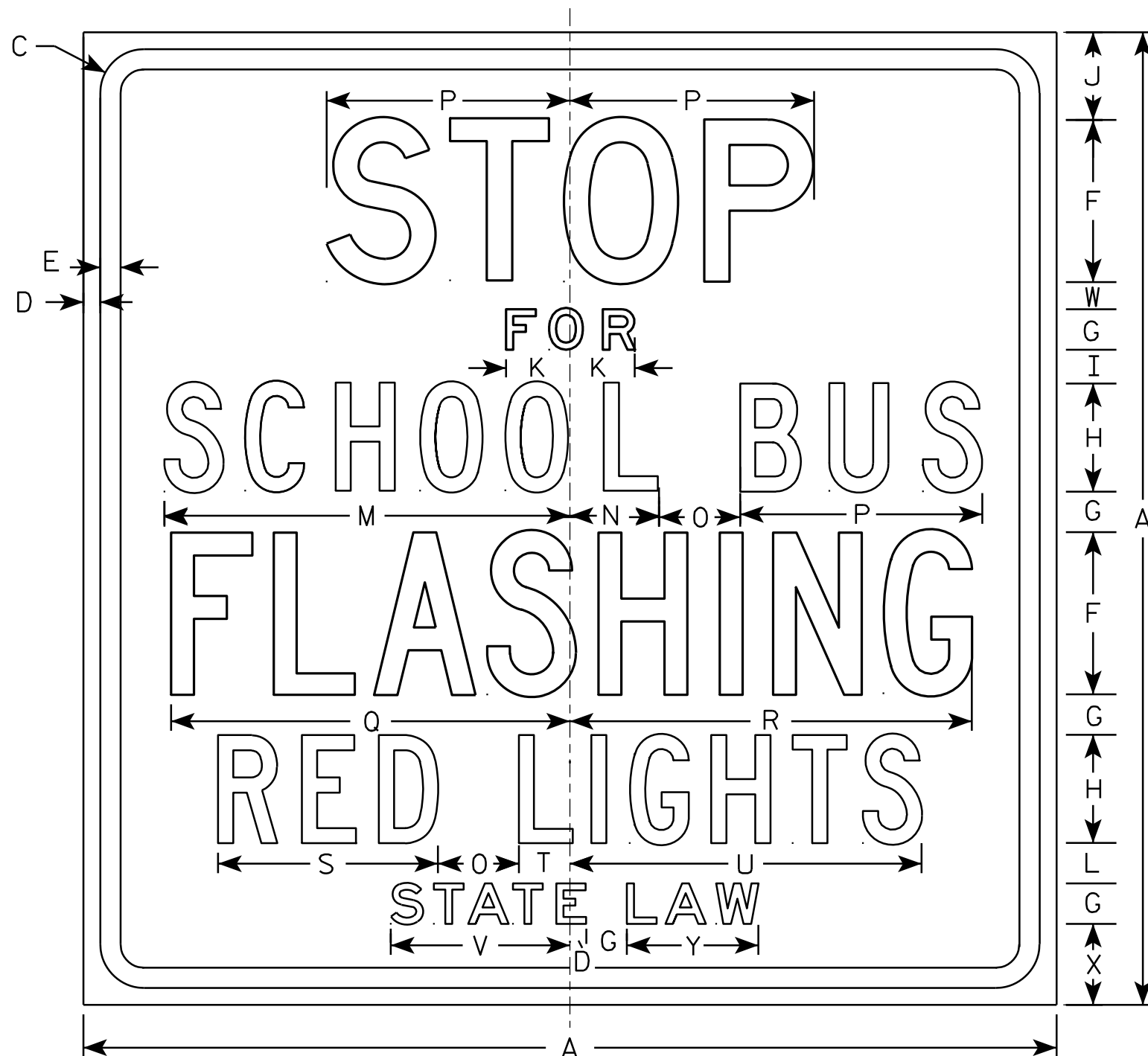
STANDARD SIGN

R10-64 & R10-64F

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/4/11 PLATE NO. R10-64.7



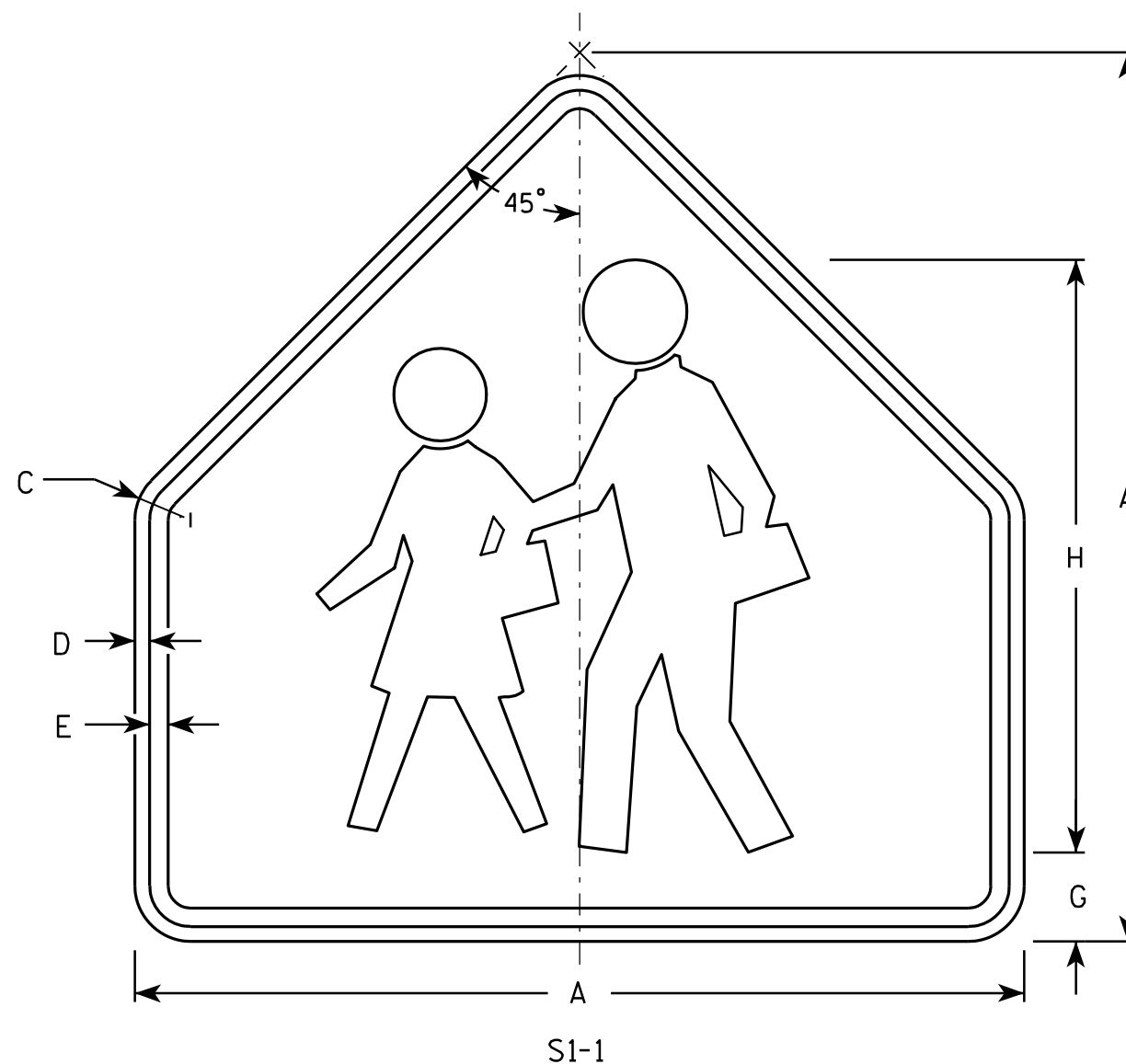
R59-51

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D
Lines 2 & 6 are Series E
Line 3, 4 & 5 are Series C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5⁄8	5⁄8	3⁄4	6	1 1⁄2	4	1 1⁄4	3 1⁄4	2 3⁄8	1 1⁄2	15	3 1⁄4	3	9	14 3⁄4	14 7⁄8	8 1⁄8	1 7⁄8	13	6 5⁄8	1	3	4 7⁄8		9.0
2M	36		1 5⁄8	5⁄8	3⁄4	6	1 1⁄2	4	1 1⁄4	3 1⁄4	2 3⁄8	1 1⁄2	15	3 1⁄4	3	9	14 3⁄4	14 7⁄8	8 1⁄8	1 7⁄8	13	6 5⁄8	1	3	4 7⁄8		9.0
3	48		2 1⁄4	3⁄4	1	8	2	6	1 1⁄4	4 3⁄4	3 1⁄4	1 1⁄2	20 1⁄4	5	3 5⁄8	12	19 1⁄2	20	11 5⁄8	3 3⁄4	19	9 1⁄2	1	3 1⁄2	6 3⁄4		16.0
4	48		2 1⁄4	3⁄4	1	8	2	6	1 1⁄4	4 3⁄4	3 1⁄4	1 1⁄2	20 1⁄4	5	3 5⁄8	12	19 1⁄2	20	11 5⁄8	3 3⁄4	19	9 1⁄2	1	3 1⁄2	6 3⁄4		16.0
5																											

STANDARD SIGN R59-51	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/30/11	PLATE NO. R59-51.10



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow-Green
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

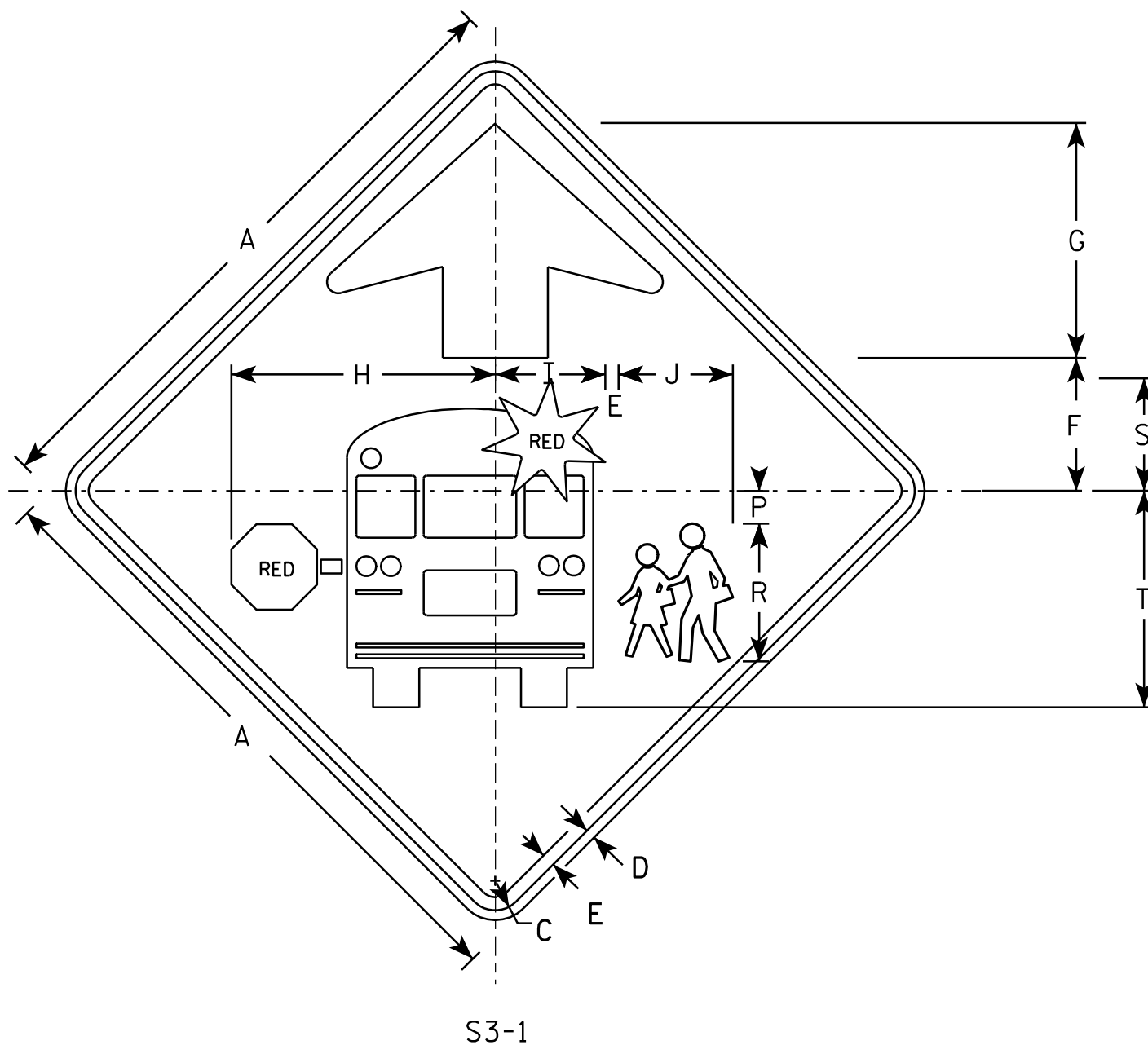
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
3	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
4	48		2 1/4	3/4	1		4 3/4	32																			12
5																											

STANDARD SIGN S1-1

WISCONSIN DEPT OF TRANSPORTATION

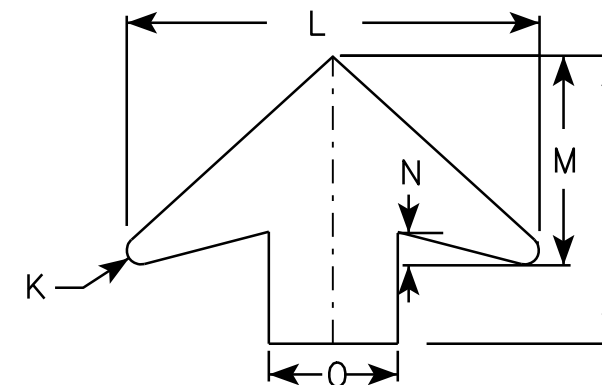
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/30/05 PLATE NO. S1-1.8

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
 - Background - YELLOW-GREEN
 - Message - BLACK except as noted
 - Circles except PEDS- RED BACKGROUND
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

[illegible]

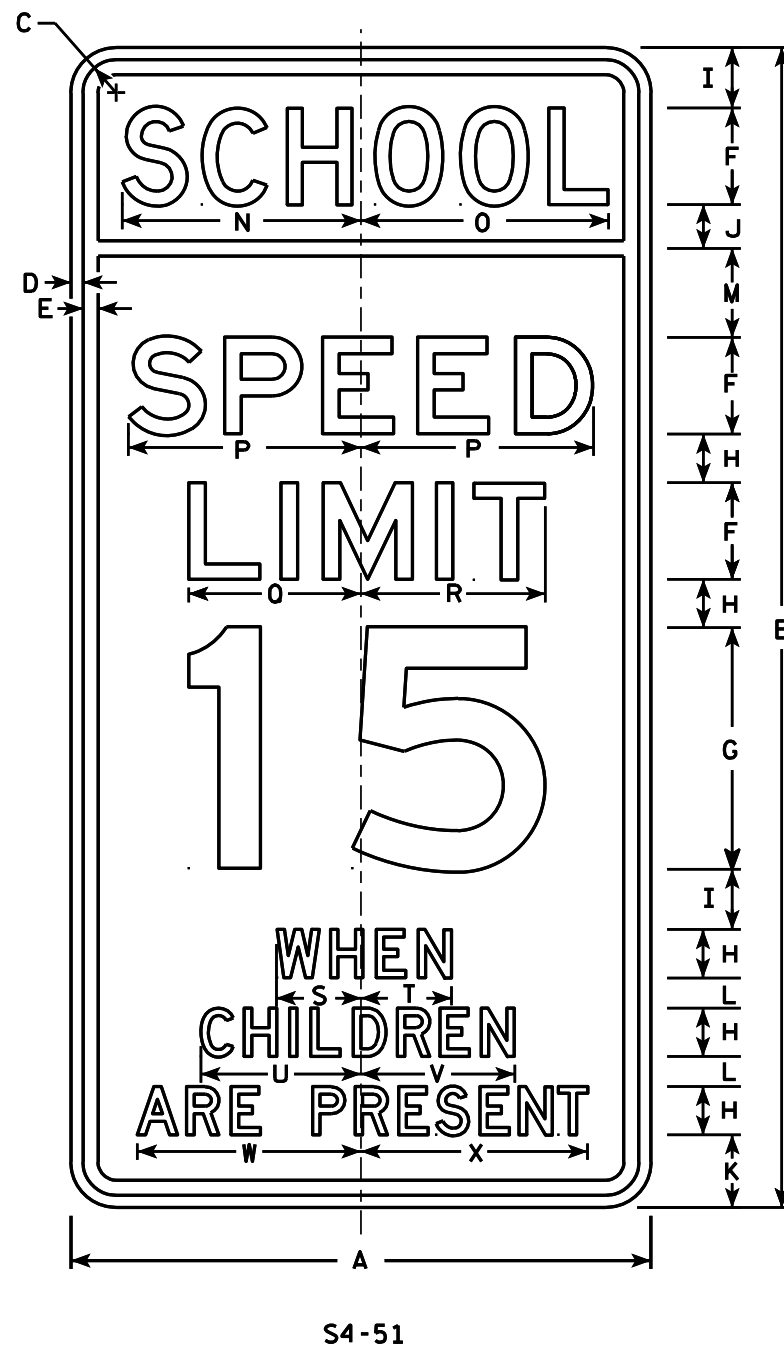
STANDARD SIGN	
S3-1	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<u>Matthew R. Rauch</u> for State Traffic Engineer
DATE <u>6/8/10</u>	PLATE NO. <u>S3-16</u>

Metric equivalent
for this sign is:

SIZE	
1	
2	600 mm X 1200 mm
3	900 mm X 1800 mm
4	
5	

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	24	48	1 ⅜	½	⅝	4	10	2	2 ½	1 ¾	3	1 ¼	3 ¾	9 ⅞	10 ¼	9 ⅝	7 ⅛	7 ⅝	3 ½	3 ⅜	6 ⅝	6 ⅜	9 ¼	9 ⅜			8.00	0.72
3	36	72	2 ¼	¾	1	6	15	3	3 ¾	2 ¾	4 ½	1 ⅞	5 ½	15	15 ¼	14 ½	11 ¼	11 ½	5 ½	5 ¾	10	9 ¾	14	14 ⅛			18.00	1.62
4																												
5																												

PROJECT NO:			HWY:			COUNTY:						SHEET NO:			E
-------------	--	--	------	--	--	---------	--	--	--	--	--	-----------	--	--	---



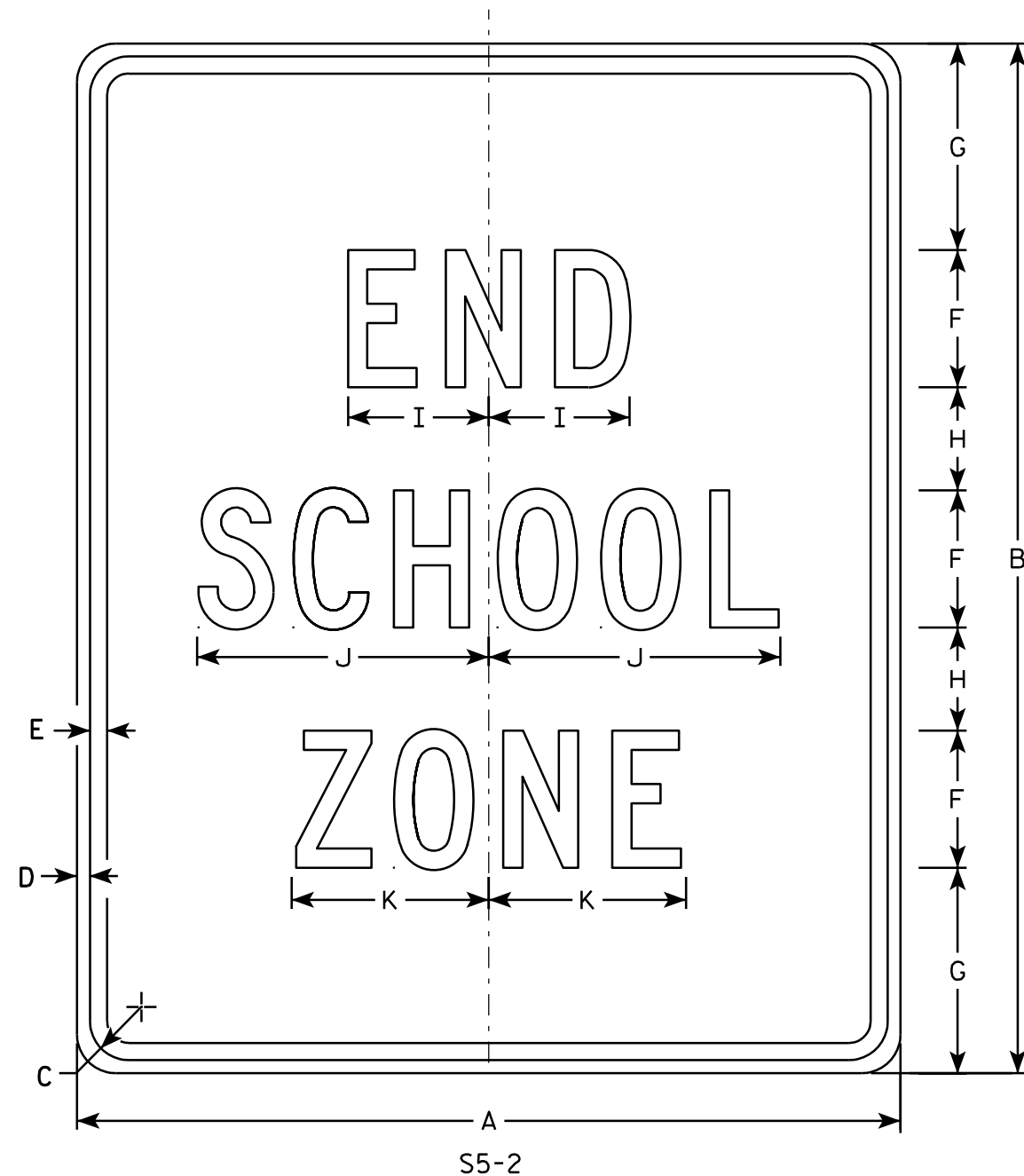
NOTES

- Sign is Type II - reference
WIS DOT Standard Specification for HIGHWAY
and STRUCTURE CONSTRUCTION latest edition. (See note 5).
- Color:
Background - See note 5
Message - Black
- Message Series - See note 6
- Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
- Top panel (SCH00L) background - Yellow Green -Type F Reflective.
Lower panel background - White -Type H Reflective.
- From top to bottom:
Lines 1, 5, 6 & 7 are series D
Lines 2, 3 & 4 are series E
- Line 4 substitute appropriate numerals and
adjust spacing to achieve proper balance.

STANDARD SIGN S4-51

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 4/26/10 PLATE NO. S4-51.9



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	30	1 1/8	3/8	1/2	4	6	3	4 1/8	8 1/2	5 3/4																5.0
3	36	42	1 5/8	5/8	3/4	6	7	5	6 1/8	12 5/8	8 5/8																10.5
4																											
5																											

STANDARD SIGN
S5-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/8/11 PLATE NO. S5-2.2

PROJECT NO:

HWY:

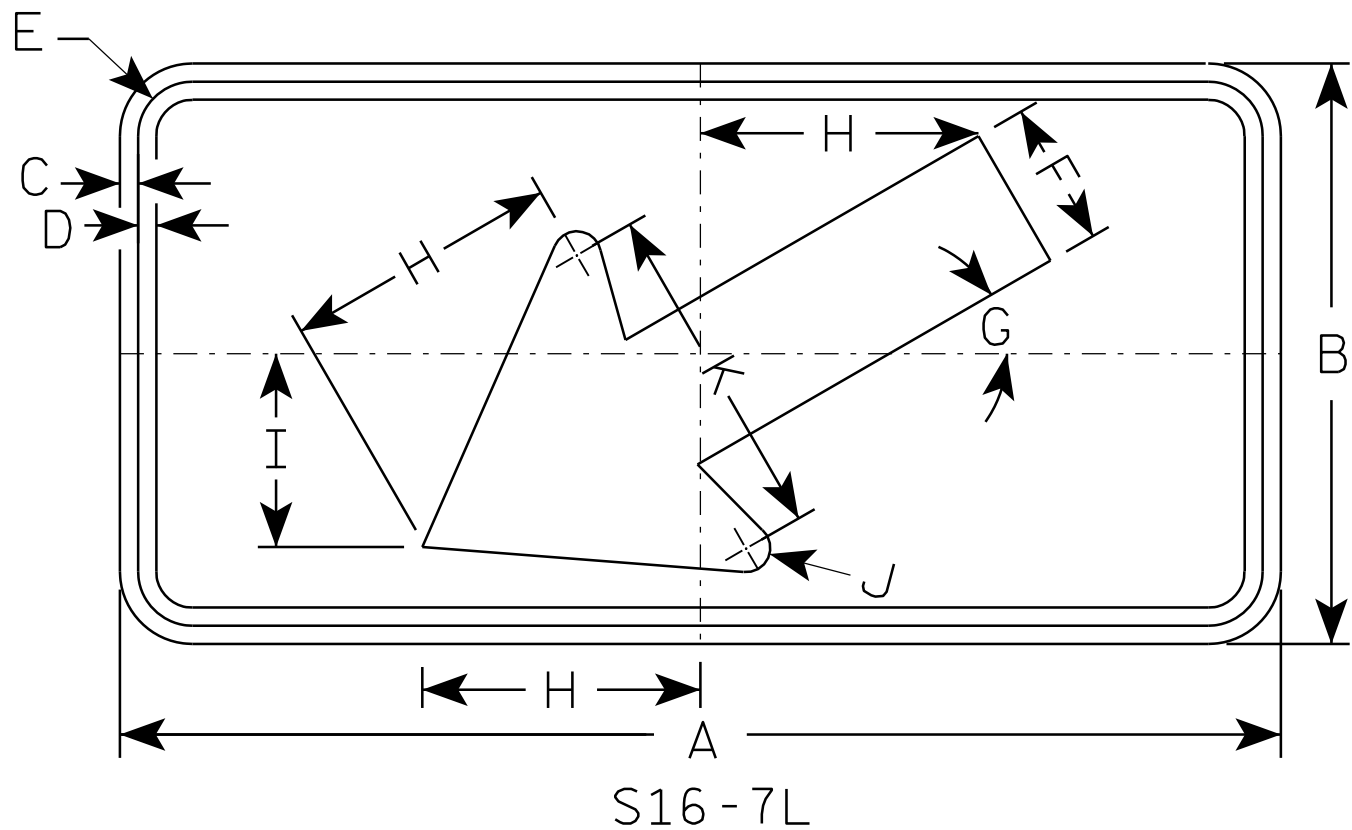
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type F Reflective -
reference WIS DOT Standard Specification for
HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow-Green
Message - Black
3. Corners may be square or rounded when base
material is plywood but borders shall be rounded
as shown. When base material is metal, the
corners and borders shall be rounded.
4. S16-7R are the same as
S16-7L except the arrow is reversed along
the vertical centerline.



S16 - 7L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	12	3/8	3/8	1 1/8	3	30°	5 3/4	4	1/2	7																2.0
2S	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
2M	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
3	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
4	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0
5																											

STANDARD SIGN	
S16-7	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 7/22/13	PLATE NO. S16-7.1

7



S16-9P

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow-Green
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	12	1 1/8	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
2S	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
2M	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/8	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

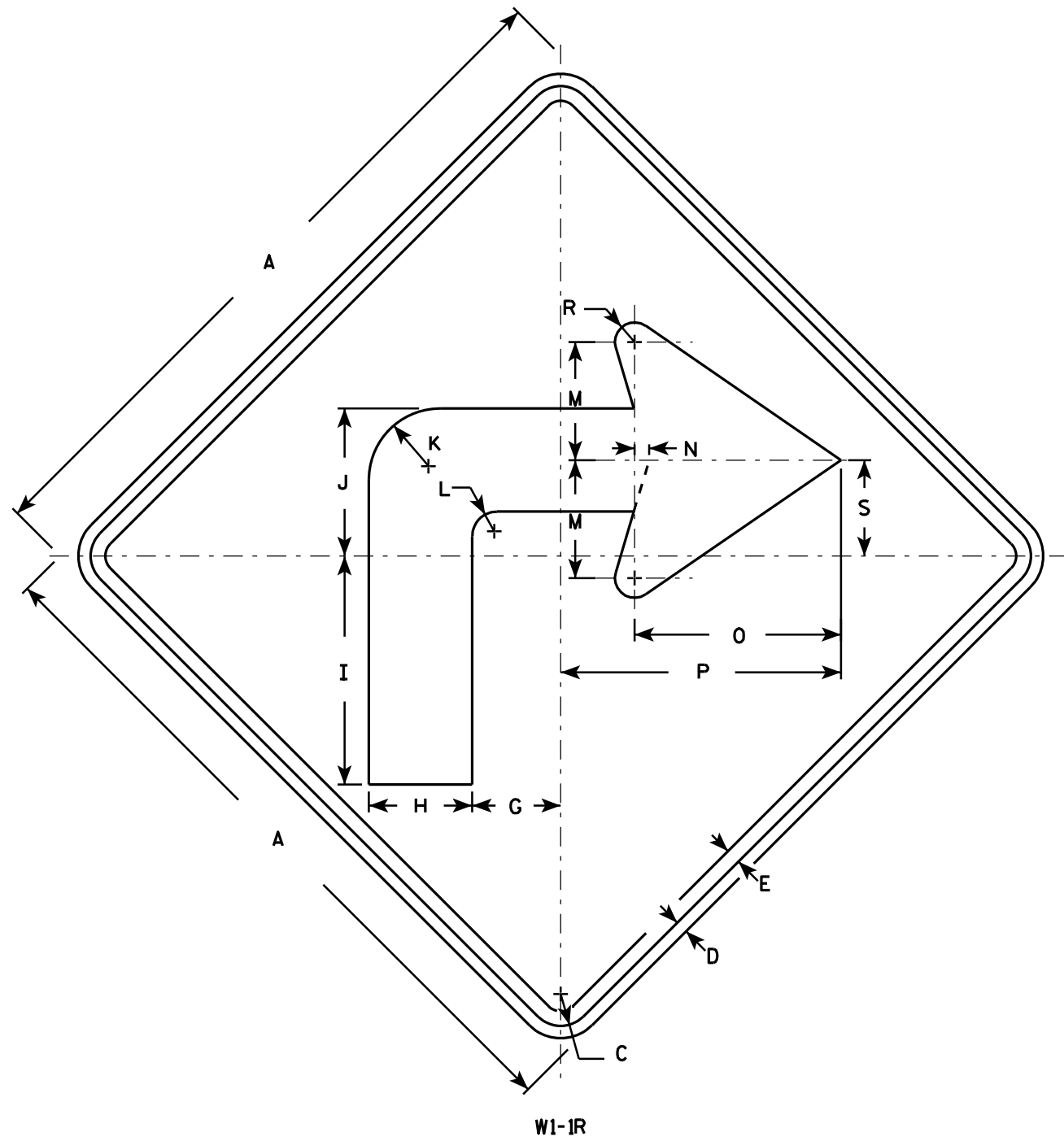
STANDARD SIGN
S16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 7/22/13 PLATE NO. S16-9P.1

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-1L is the same as W1-1R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		3	3 1/2	7 3/4	5	2 1/2	7/8	4	1/2	7	9 1/2		5/8	3 1/4								4.0
2S	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
2M	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
3	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
4	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0
5	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0

STANDARD SIGN

W1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

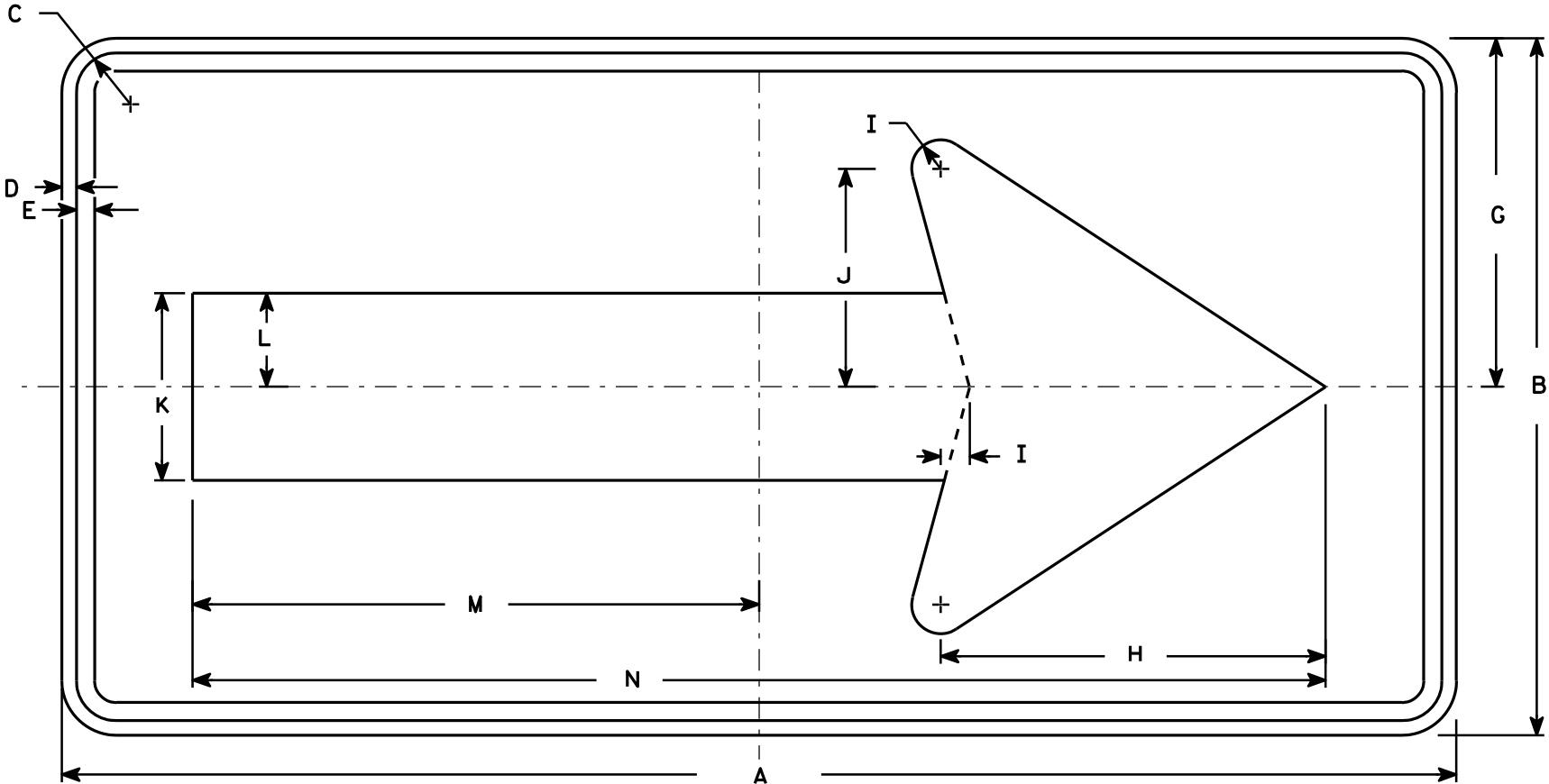
DATE 5/15/12 PLATE NO. W1-1.11

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:

Background - Yellow

Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



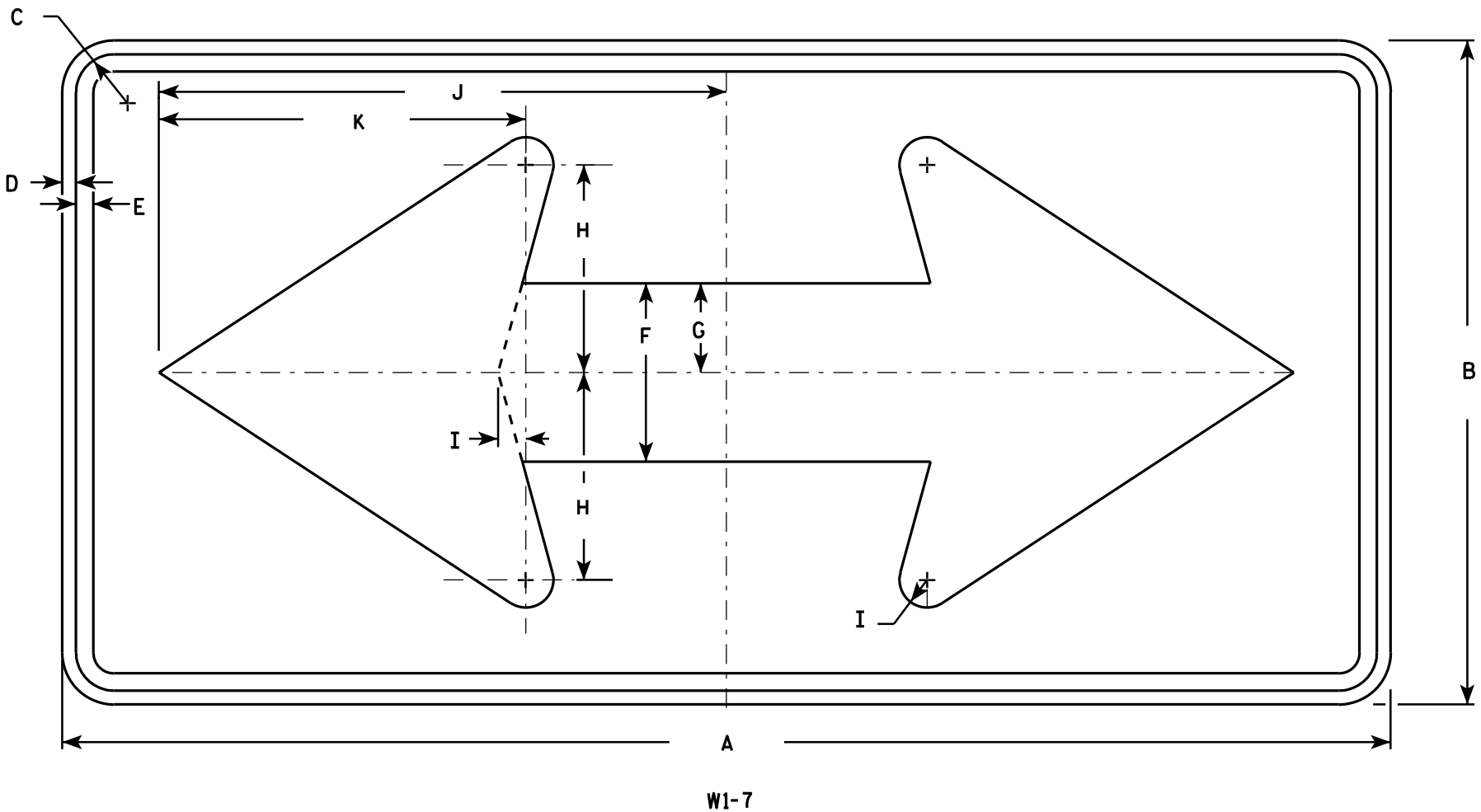
W1-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	3/8		9	10	3/4	5 5/8	4 3/4	2 3/8	14 5/8	29 1/4													4.5
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	96	48	2 1/4	3/4	1		24	26 1/2	2	15	13	6 1/2	39	78													32.0

STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/7/10 PLATE NO. W1-6.8



NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

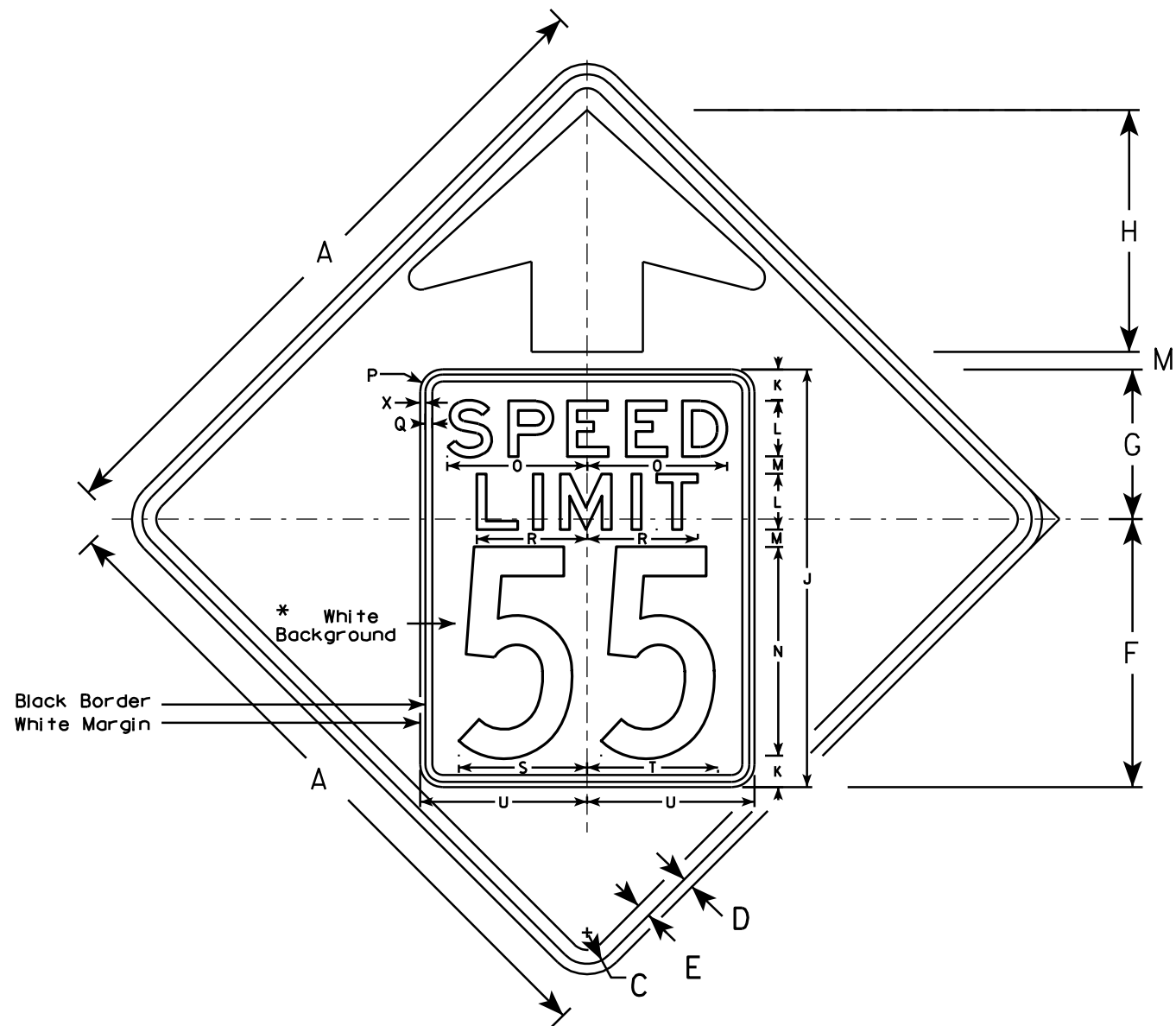
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 1/2	5 3/4	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

STANDARD SIGN
W1 - 7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-7.7

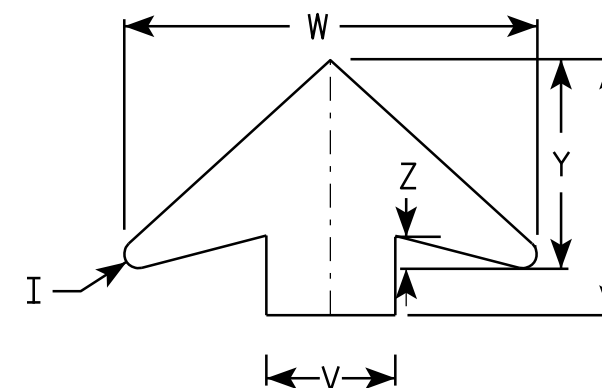


W3-5

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color: *
Background - YELLOW*
Message - BLACK
3. Message Series - C for numbers Series E for wording
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
2M	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
3	36		1 5⁄8	5⁄8	3⁄4	14 1⁄2	9 1⁄2	11 1⁄2	5⁄8	24	2	3	1	12	7 1⁄8	1 1⁄2	3⁄8	5 3⁄4	7 1⁄4	7 1⁄8	9	6	19 1⁄4	3⁄8	9 3⁄4	1 5⁄8	9.0
4	48		2 1⁄4	3⁄4	1	19 1⁄4	10 3⁄4	17 3⁄8	7⁄8	30	2 1⁄4	4	1 1⁄4	15	10	1 5⁄8	1⁄2	8	9 1⁄4	9 3⁄8	12	8	25 5⁄8	3⁄8	13	2	16.0
5	48		2 1⁄4	3⁄4	1	19 1⁄4	10 3⁄4	17 3⁄8	7⁄8	30	2 1⁄4	4	1 1⁄4	15	10	1 5⁄8	1⁄2	8	9 1⁄4	9 3⁄8	12	8	25 5⁄8	3⁄8	13	2	16.0

STANDARD SIGN

W3-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

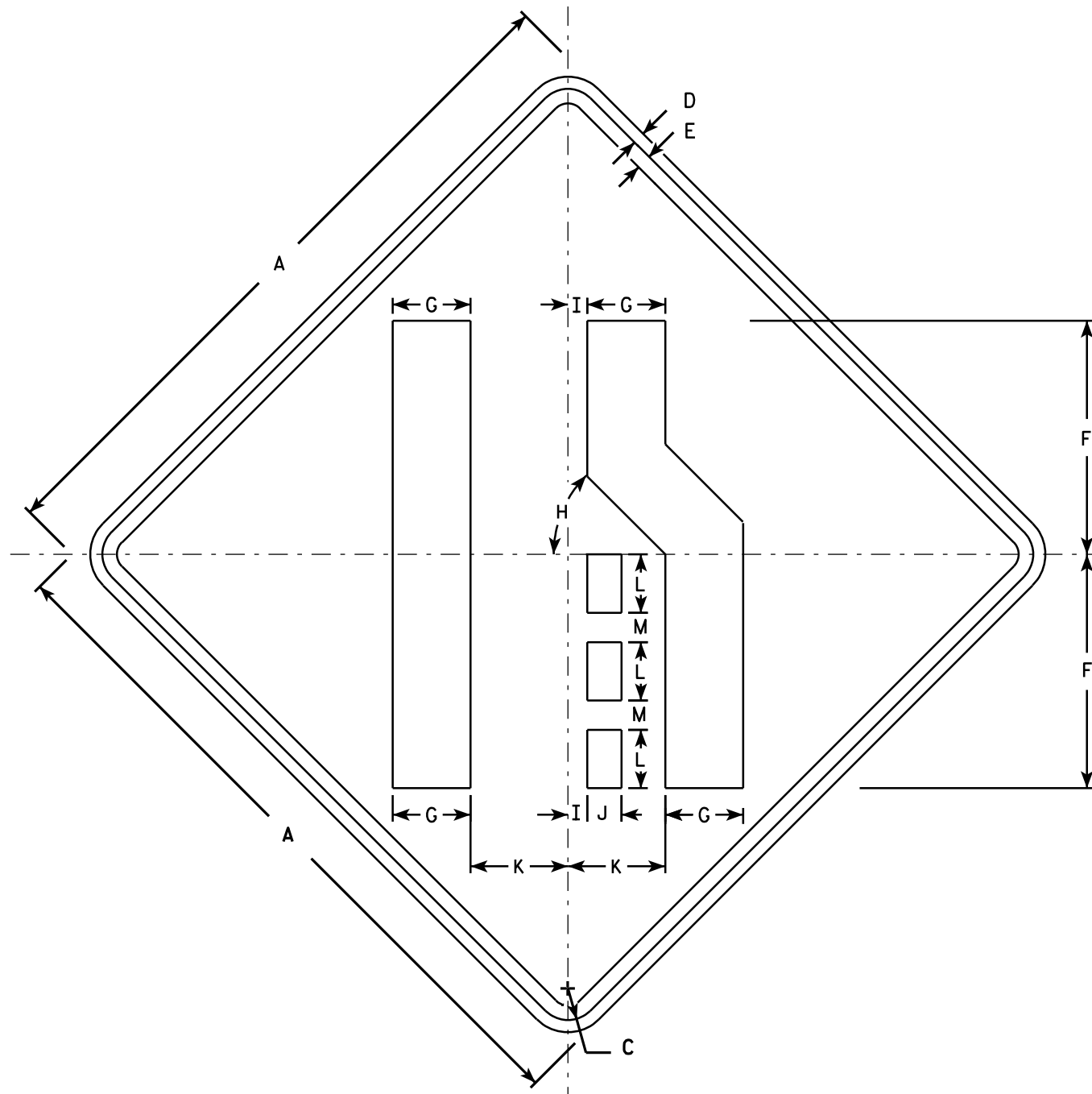
DATE 5/29/12

PLATE NO. W3-5.5

PROJECT NO:

SHEET NO:

E



W4-2R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W4-2L is the same as W4-2R except the symbols is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3⁄8	1⁄2	5⁄8	10	3 3⁄8	45°	7⁄8	1 1⁄2	4 1⁄4	2 1⁄2	1 1⁄4														6.25
2S	36		1 5⁄8	5⁄8	3⁄4	12	4	45°	1	1 3⁄4	5	3	1 1⁄2														9.0
2M	36		1 5⁄8	5⁄8	3⁄4	12	4	45°	1	1 3⁄4	5	3	1 1⁄2														9.0
3	36		1 5⁄8	5⁄8	3⁄4	12	4	45°	1	1 3⁄4	5	3	1 1⁄2														9.0
4	48		2 1⁄4	3⁄4	1	16	5 3⁄8	45°	1 1⁄4	2 3⁄8	6 3⁄4	4	2														16.0
5	48		2 1⁄4	3⁄4	1	16	5 3⁄8	45°	1 1⁄4	2 3⁄8	6 3⁄4	4	2														16.0

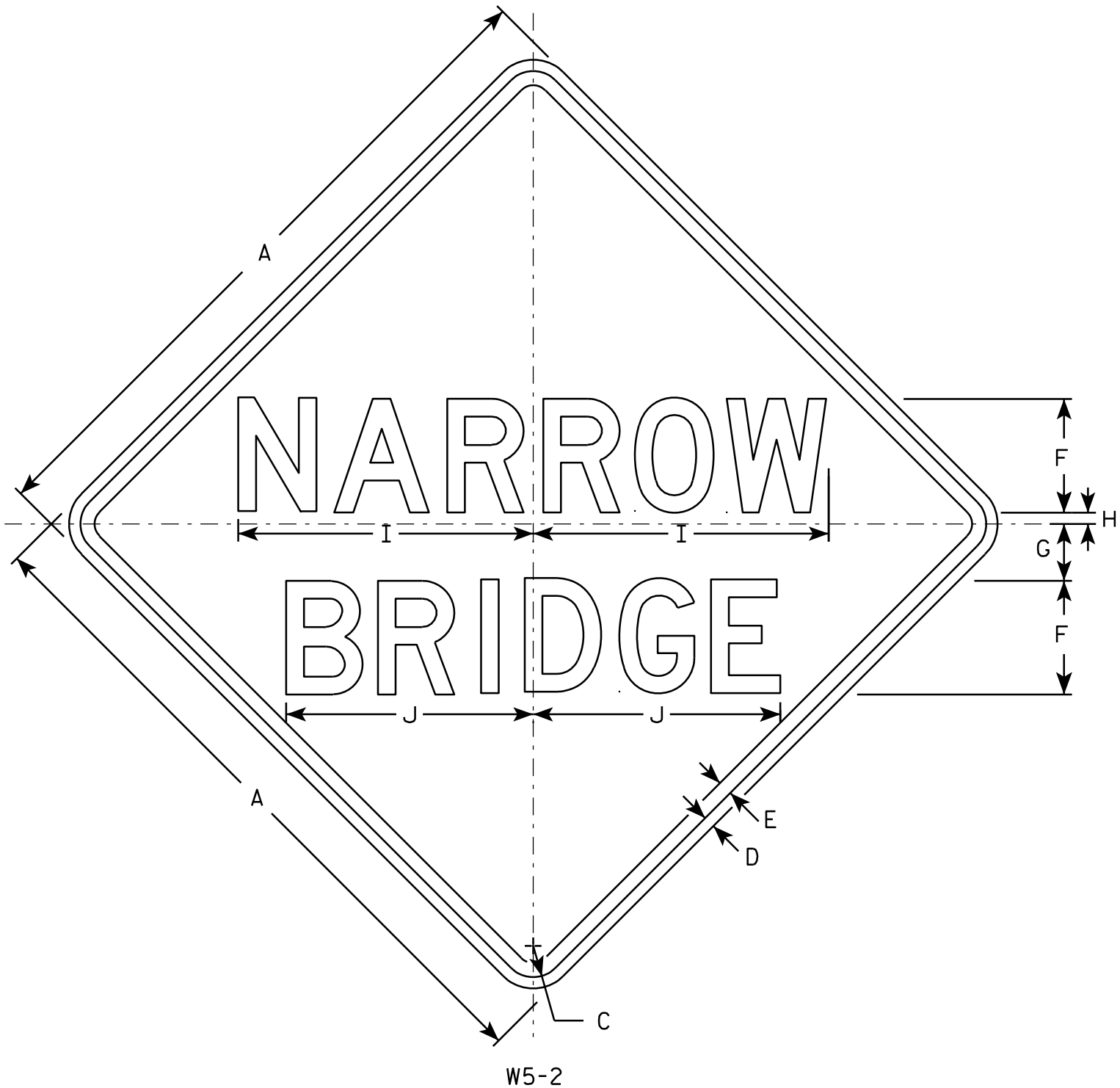
STANDARD SIGN W4-2

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 3/12/13 PLATE NO. W4-2.14

PROJECT NO:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3⁄8	1⁄2	5⁄8	5	2 1⁄2	1⁄2	13	10 7⁄8																	6.25
2S	36		1 5⁄8	5⁄8	3⁄4	6	3	3⁄4	15 5⁄8	13 1⁄8																	9.0
2M	36		1 5⁄8	5⁄8	3⁄4	6	3	3⁄4	15 5⁄8	13 1⁄8																	9.0
3	36		1 5⁄8	5⁄8	3⁄4	6	3	3⁄4	15 5⁄8	13 1⁄8																	9.0
4	48		2 1⁄4	3⁄4	1	8	4	3⁄4	20 3⁄4	17 3⁄8																	16.0
5																											

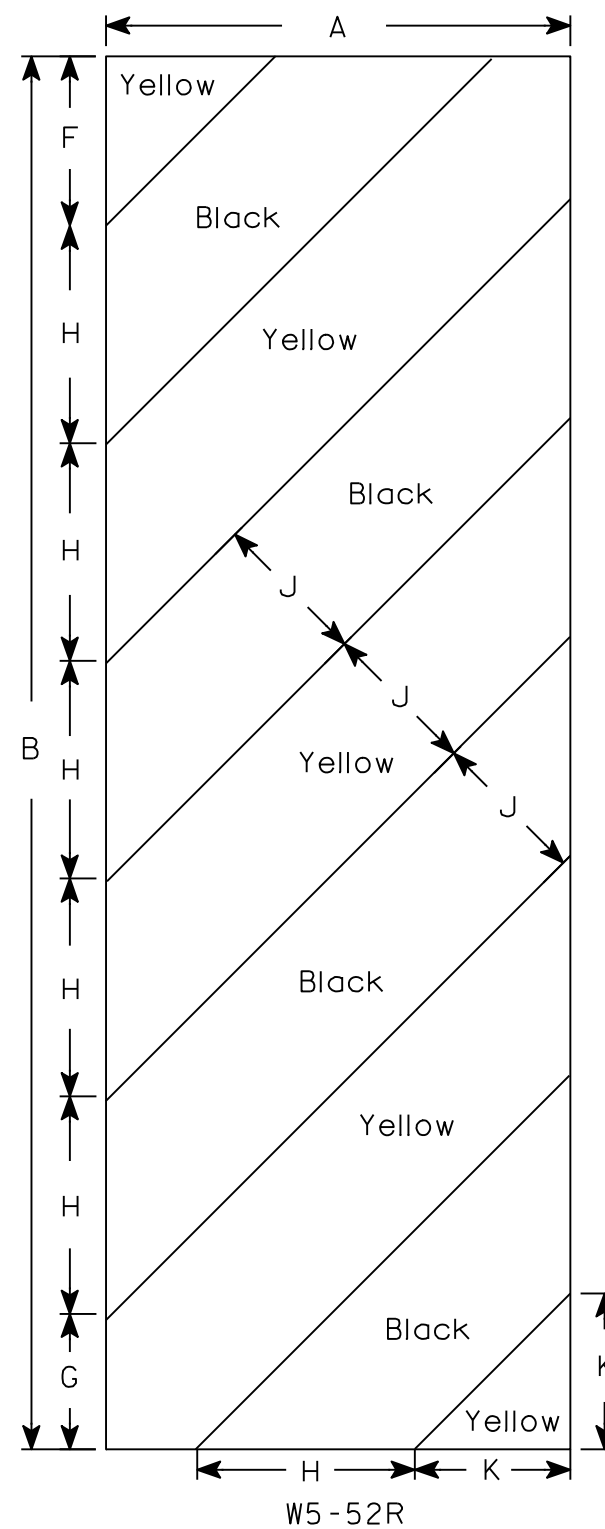
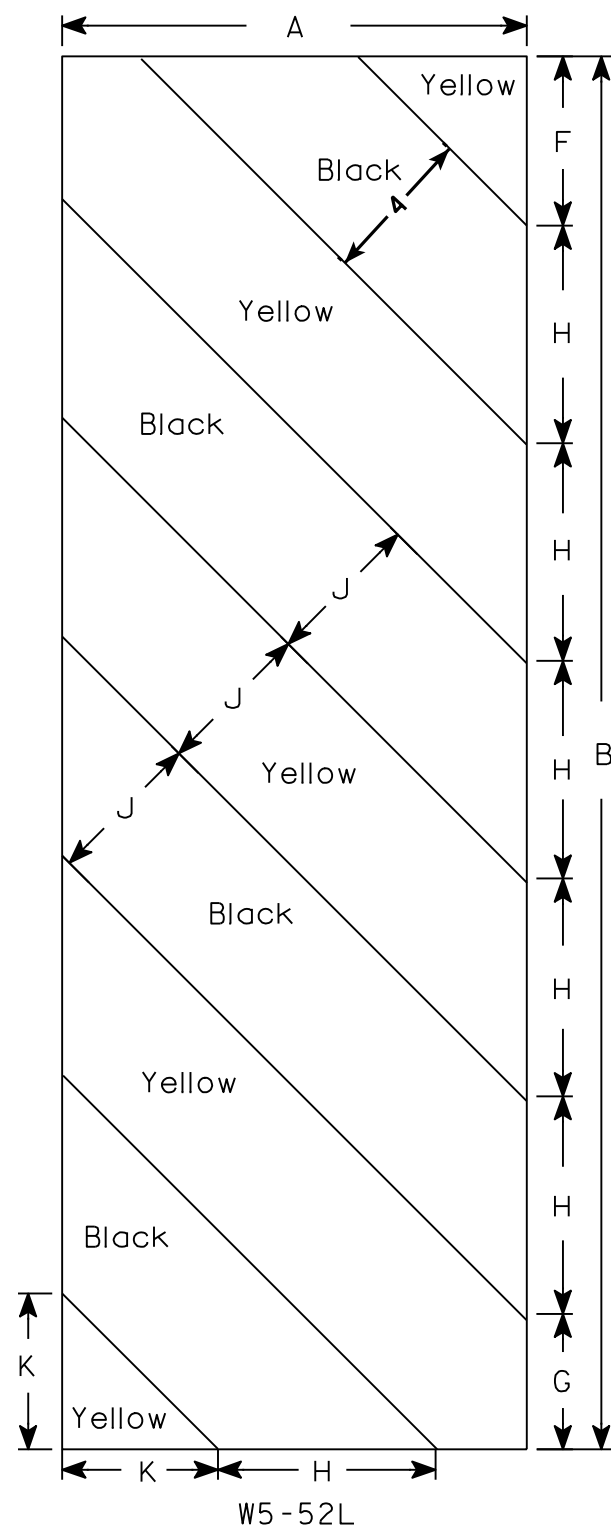
STANDARD SIGN
W5-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 03/12/13 PLATE NO. W5-2.8

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
 - Background - Yellow
 - Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

[illegible]

STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 5/29/12 PLATE NO. W5-52.9

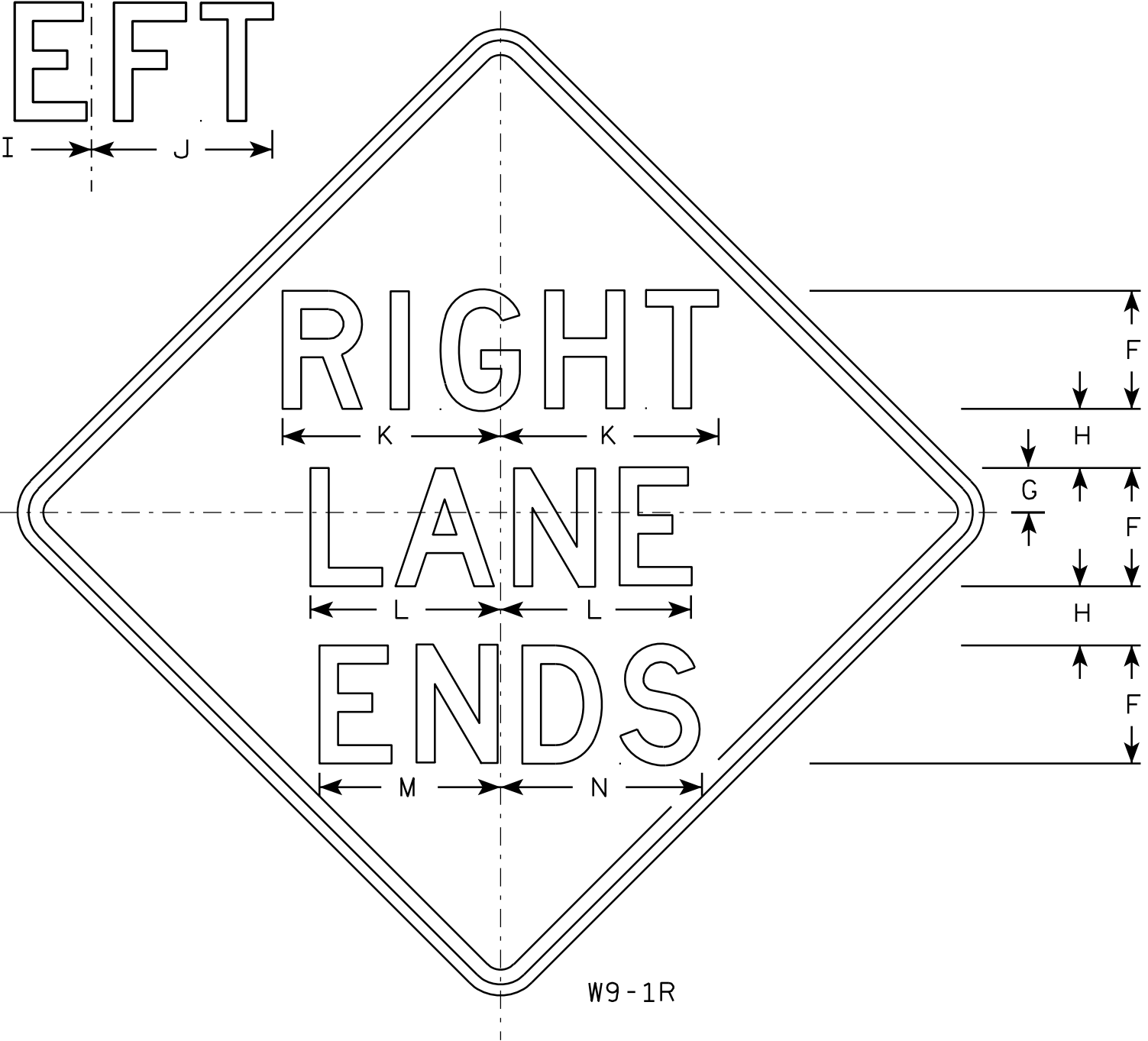
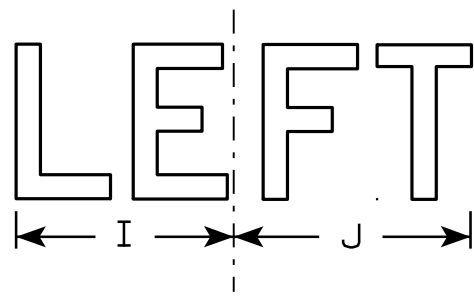
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



W9-1R

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Message Series - D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. W9-1L same as W9-1R except the word Left replaces Right.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	5	1 1/2	2 1/2	7 1/8	7 5/8	9 1/4	8 1/8	7 5/8	8 5/8													6.25
2S	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
2M	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
3	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
4	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
5	48		2 1/4	3/4	1	8	3	4	11 1/4	12 1/4	14 3/4	12 7/8	12 1/4	13 5/8													16.0

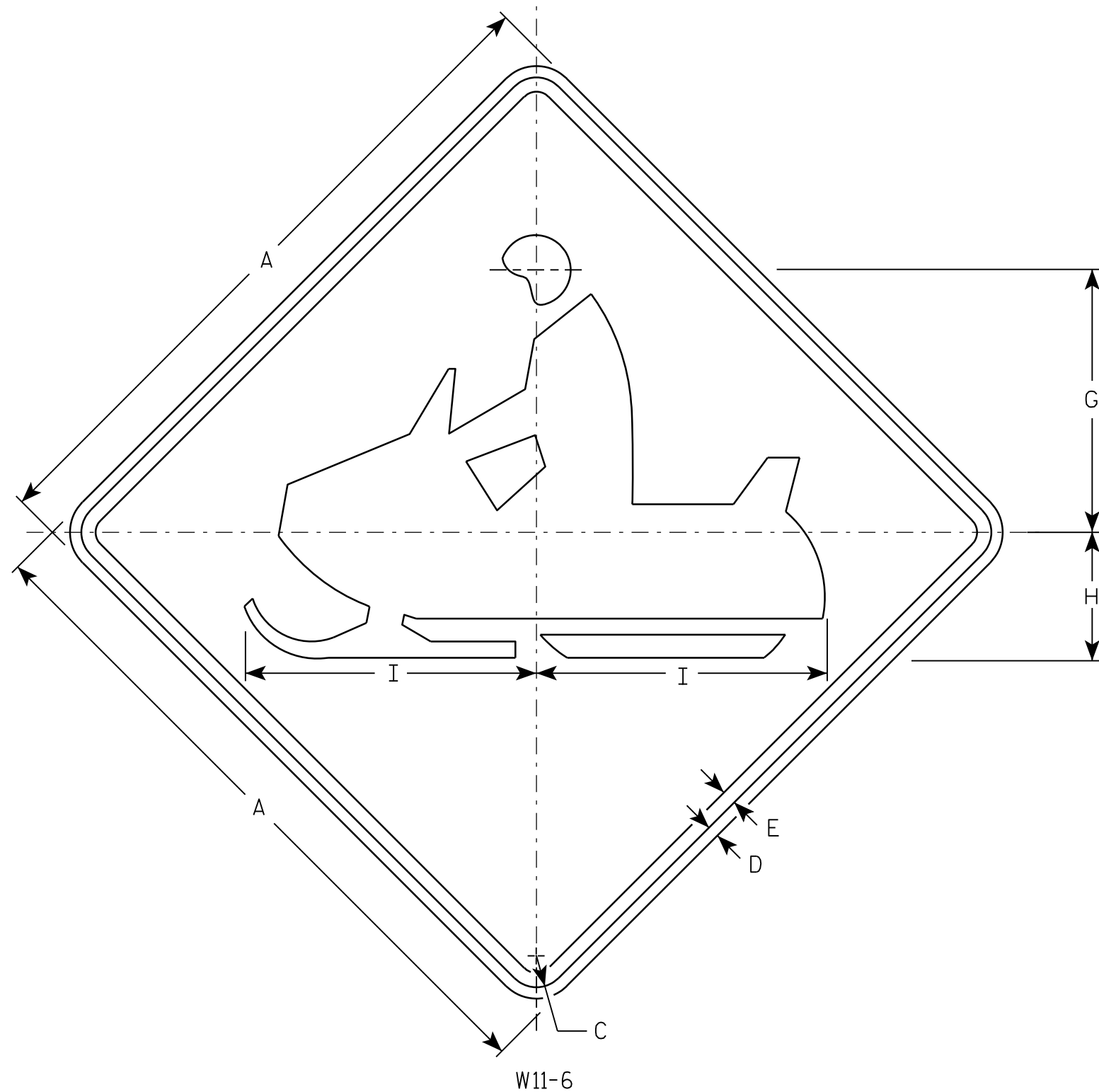
STANDARD SIGN

W9-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 03/18/13 PLATE NO. W9-1.8



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

W11-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		9 1/2	4 1/2	10 1/4																		4.0
2S	30		1 3/8	1/2	5/8		11 1/2	5 5/8	12 3/4																		6.25
2M	30		1 3/8	1/2	5/8		11 1/2	5 5/8	12 3/4																		6.25
3	36		1 5/8	5/8	3/4		14 1/8	6 3/4	15 1/4																		9.0
4	48		2 1/4	3/4	1		19	9	20 1/2																		16.0
5																											

STANDARD SIGN W11-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch
for State Traffic Engineer

DATE 3/13/13 PLATE NO. W11-6.8

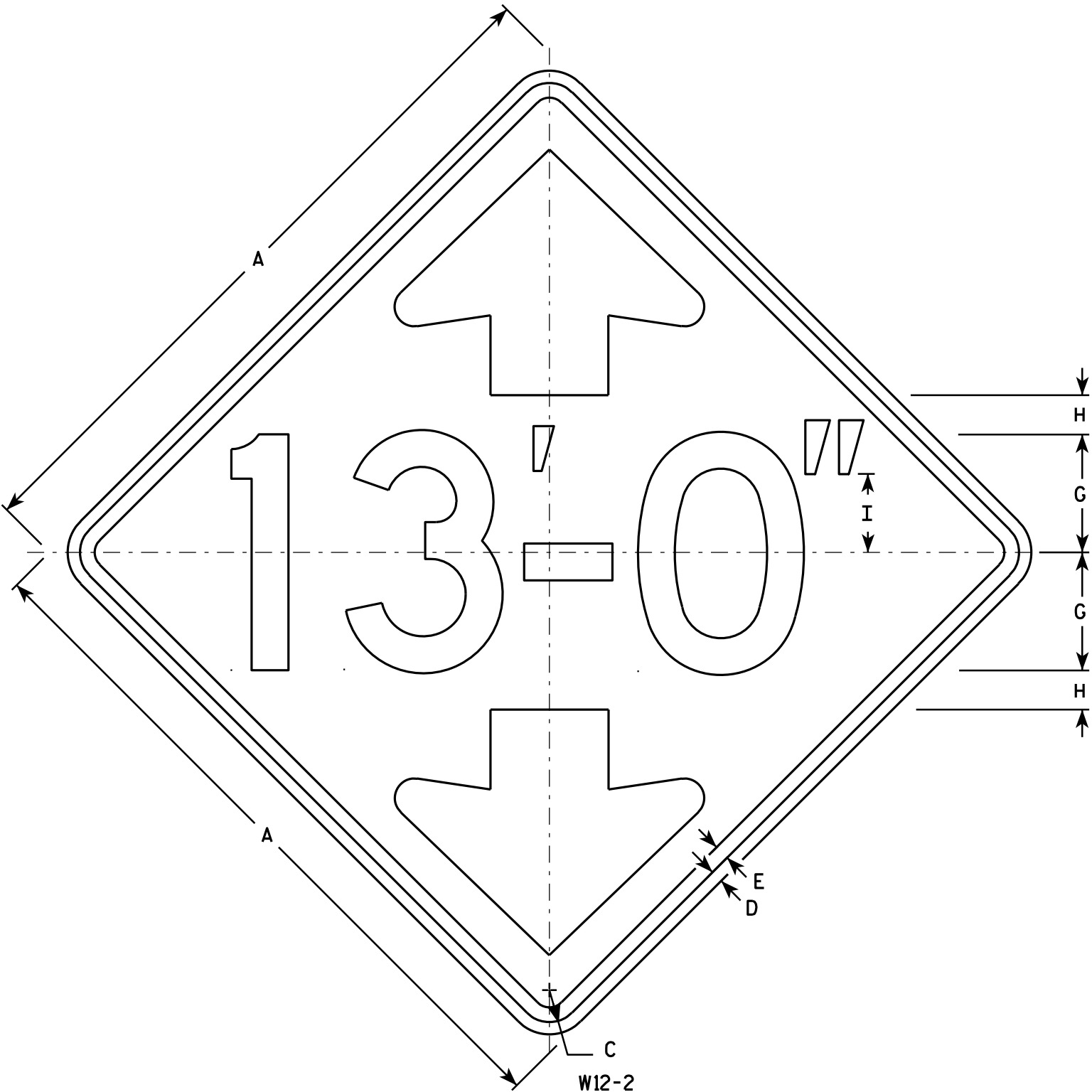
PROJECT NO:

HWY:

COUNTY:

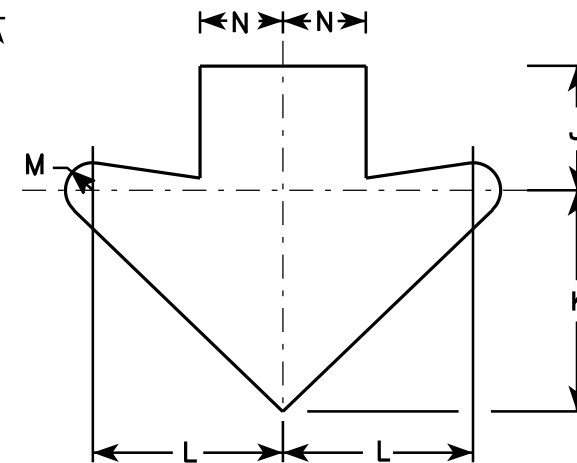
SHEET NO:

E

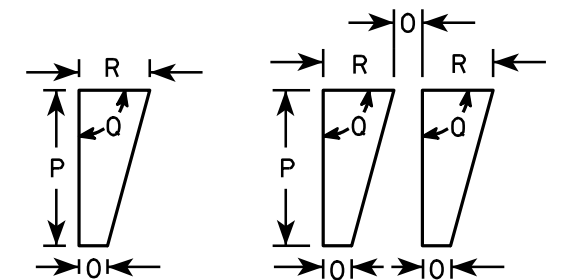


NOTES

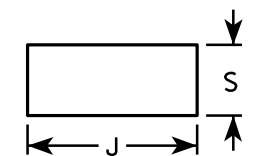
1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing of numerals, hyphen, foot & inch marks to achieve proper balance.



Arrow Detail



Foot Mark & Inch Mark Detail

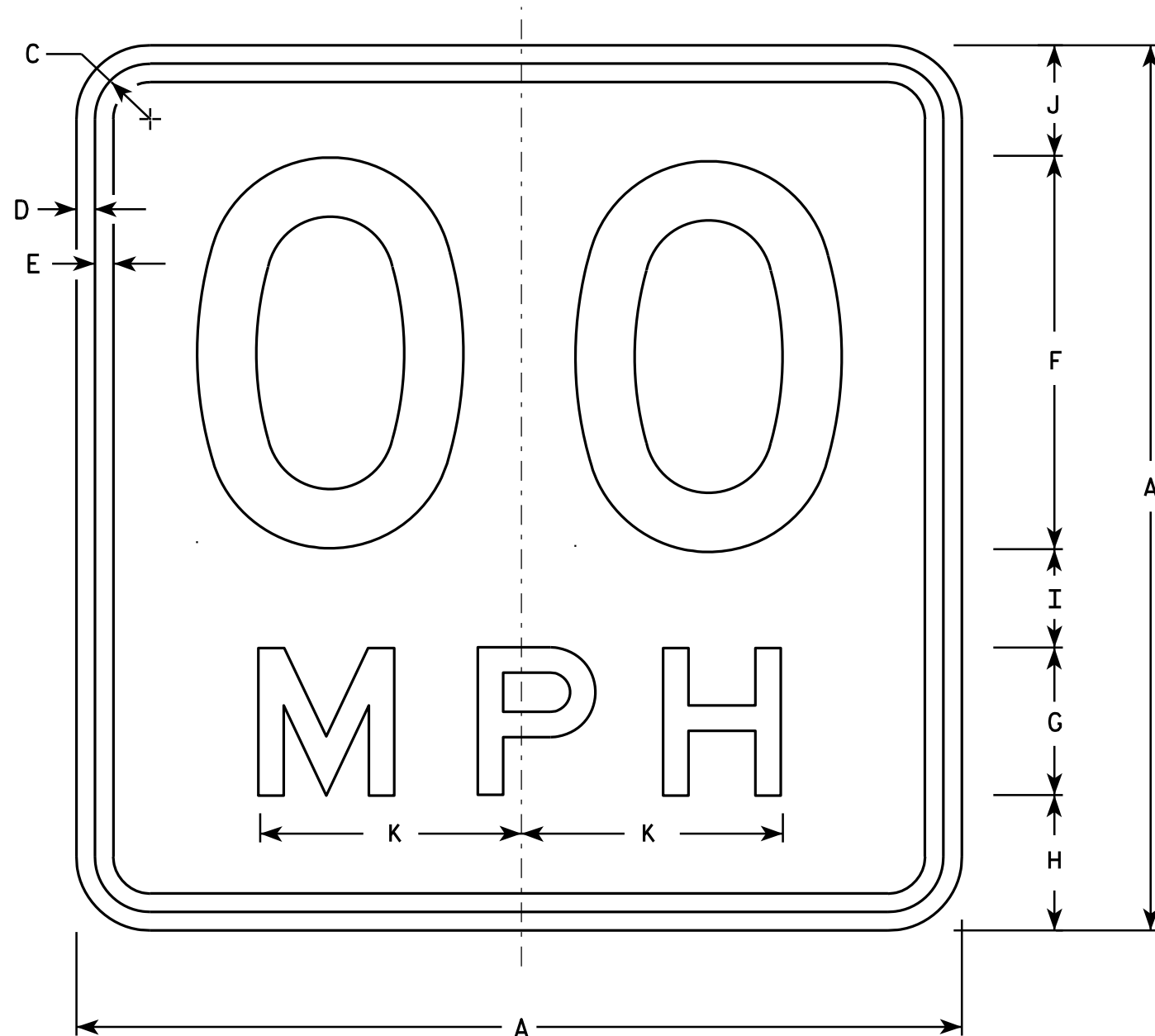


Hyphen Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		5	1 5/8	3 3/8	3 3/4	6 5/8	5 3/4	3/4	2 1/2	1/2	2 1/4	90°	1	1 5/8								6.25
2S	36		1 5/8	5/8	3/4		6	2	4	4 1/2	8	6 7/8	1	3	1/2	2 3/4	90°	1 1/4	1 7/8								9.00
2M	36		1 5/8	5/8	3/4		6	2	4	4 1/2	8	6 7/8	1	3	1/2	2 3/4	90°	1 1/4	1 7/8								9.00
3	36		1 5/8	5/8	3/4		6	2	4	4 1/2	8	6 7/8	1	3	1/2	2 3/4	90°	1 1/4	1 7/8								9.00
4	36		1 5/8	5/8	3/4		6	2	4	4 1/2	8	6 7/8	1	3	1/2	2 3/4	90°	1 1/4	1 7/8								9.00
5	48		2 1/4	3/4	1		8	2 5/8	5 1/2	5 7/8	10 5/8	9 1/4	1 3/8	4	5/8	3 5/8	90°	1 5/8	2 1/2								16.00

STANDARD SIGN W12-2

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 3/13/13 PLATE NO. W12-2.9



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

W13-1

- * For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area Sq. Ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

PROJECT NO:

HWY:

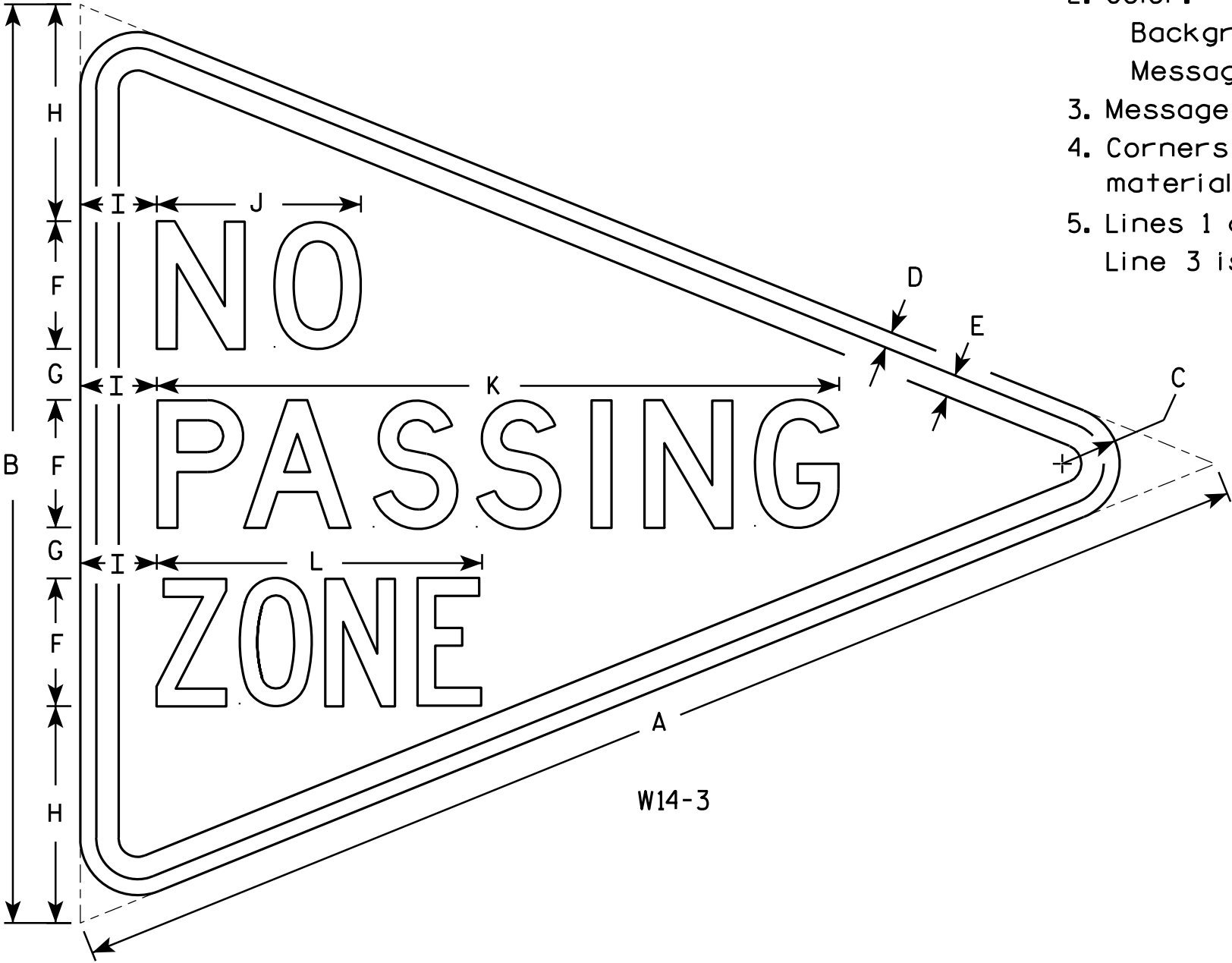
COUNTY:

SHEET NO:

E

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Message Series - See note 5
- 4. Corners and borders shall be rounded on all base materials for this sign.
- 5. Lines 1 and 2 are Series D.
Line 3 is series C.



W14-3

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
2M	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															6.0
3	64	48	3	3/4	1 1/4	6	3	12	4	10 3/4	33 5/8	16 1/2															10.7
4																											
5																											

STANDARD SIGN
W14-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W14-3.9

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

Notes



Wisconsin Department of Transportation

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