SWL JAN 2017

PROJECT ID: 5990-00-36

WITH: N/A

COUNTY: ROCK

ORDER OF SHEETS

TOTAL SHEETS = 156

05

N

DESIGN DESIGNATION

A.A.D.T.	2017	= 2,200
A.A.D.T.	2037	= 3,300
D.H.V.		= 430
D.D.		= 58/42
T.		= 3.4
DESIGN SPEED		= 40 MPH
ESALS		= 290,000

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

CITY OF JANESVILLE, AUSTIN ROAD

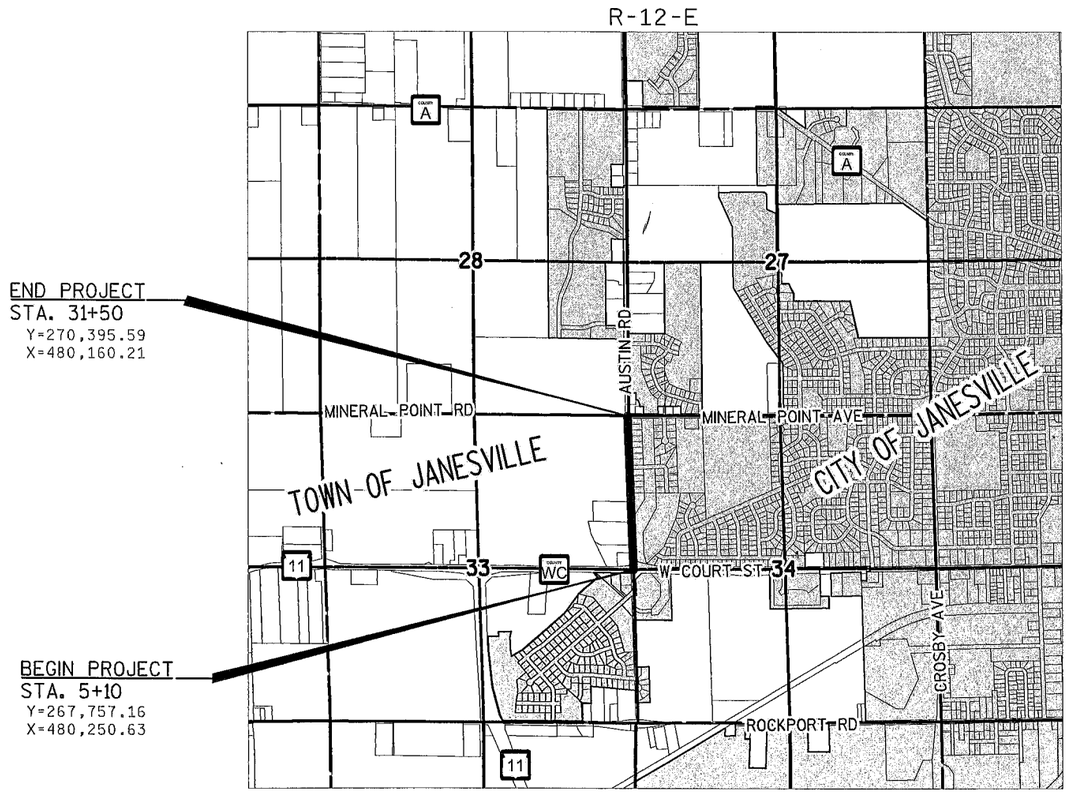
(WEST COURT STREET to MINERAL POINT AVENUE)

LOCAL STREET

ROCK COUNTY

STATE PROJECT NUMBER
5990-00-36

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5990-00-36	WISC 2017004	1



TOTAL NET LENGTH OF CENTERLINE = 0.500

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, ROCK COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ACCEPTED FOR CITY OF JANESVILLE

7/20/2016 (Date) Mike Payne (CITY ENGINEER)

ORIGINAL PLANS PREPARED BY

Batterman
engineers surveyors planners

R.H. BATTERMAN & CO., INC.
2857 BARTELLS DRIVE
BELOIT, WI 53511
P 608.365.4464
TF 877.457.2235
F 608.365.1850

WISCONSIN PROFESSIONAL ENGINEER
TODD W. NEEDHAM
E-38332
JANESVILLE
WI

DATE: 7/20/16 (PROFESSIONAL ENGINEER)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	BATTERMAN
Designer	BATTERMAN
Management Consultant	KL ENGINEERING, INC

APPROVED FOR THE DEPARTMENT

DATE: 7/28/16 (Management Consultant Signature)

E

GENERAL NOTES

ELEVATIONS SHOWN ON THE PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 88 (2007).

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE, UTILITIES AND LOCAL GOVERNMENT BEFORE THE START OF CONSTRUCTION WORK.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE AND IS NOT SHOWN ON THE CROSS SECTIONS, BUT IS MEASURED AND PAID FOR AS EXCAVATION COMMON. THE LOCATION OF EBS SHALL BE DETERMINED BY THE ENGINEER.

SELECT CRUSHED MATERIAL SHALL BE USED IN ALL EBS AREAS.

RADII DIMENSIONS FOR THE CURB & GUTTER ARE TO THE FLANGE OF THE CURB. CURB & GUTTER PLAN GRADES ARE AT THE FLANGE LINE UNLESS OTHERWISE NOTED.

THE EXACT LOCATIONS OF ALL DRIVEWAY ENTRANCES ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE EROSION CONTROL FEATURES ARE SHOWN ON THE PLAN AND ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS TO BE DETERMINED BY THE ENGINEER. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH A TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY SHALL BE RESTORED AS DIRECTED BY THE ENGINEER.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

DETAILS OF CONSTRUCTION NOT SHOWN ON THE PLAN SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

RESTORATION OF EXPOSED SLOPE AND DITCHES SHALL TAKE PLACE NOT MORE THAN 7 DAYS AFTER FINISHED GRADING IS COMPLETE.

THE CONTRACTORS PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINT FROM BEING LOCATED WITHIN A DRIVING OR TURNING LANE.

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 110 LB/SY/INCH.

ALL INLET RIM GRADES ARE TO THE FLANGE OF CURB.

OFFSETS FOR INLETS AND MANHOLES ARE TO CENTER OF STRUCTURE. OFFSETS FOR APRON ENDWALLS ARE TO END OF PIPE.

THE CONTRACTOR SHALL COORDINATE ALL UTILITY ADJUSTMENTS WITH THE APPROPRIATE UTILITY.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTY OWNERS ALONG THE PROJECT AT ALL TIMES.

ASPHALTIC SURFACE SHALL USE 9.5 mm MAX NOMINAL SIZE GRADATION.

ORDER OF DETAIL SHEETS

UTILITIES

WISCONSIN POWER & LIGHT (GAS & ELECTRIC)
ATTN: JANE BALLMER
3730 KENNEDY ROAD
JANESVILLE, WI 53545
TELEPHONE: (608)757-7517
MOBILE: (608)751-7935
EMAIL: JANEBALLMER@ALLIANTENERGY.COM

CHARTER COMMUNICATIONS (CABLE TELEVISION)
ATTN: RANDY STEURER
1348 PLAINFIELD AVENUE
JANESVILLE, WI 53545
TELEPHONE: (608)373-7544
MOBILE: (608)209-3194
EMAIL: RSTEURER@CHARTERCOMM.COM

AT&T WISCONSIN (TELEPHONE)
ATTN: CAROL ANASON
316 W WASHINGTON AVENUE
MADSION WI 53703
TELEPHONE: (608)252-2385
EMAIL: CA2624@ATT.COM

CITY OF JANESVILLE (STREET LIGHTING)
ATTN: DENNIS RYAN, P.E.
18 N JACKSON STREET
P.O. BOX 5005
JANESVILLE, WI 53545-5005
TELEPHONE: (608)755-3171
EMAIL: RYAND@CI.JANESVILLE.WI.US

CITY OF JANESVILLE (SEWER AND WATER)
ATTN: CRAIG THIESENHUSEN
123 EAST DELAVAN DRIVE
JANESVILLE, WI 53545
TELEPHONE: (608)755-3115
EMAIL: THIESENHUSENC@CI.JANESVILLE.WI.US

**DENOTES UTILITIES THAT ARE NOT DIGGERS HOTLINE MEMBERS

SOIL BORING TABLE

BORING	STA.	OFF.	HMA DEPTH
SB1	7+69	2.6' RT	10"
SB2	13+00	0.8' RT	5"
SB3	18+20	0.5' LT	8"
SB4	24+22	1.4' LT	8"
SB5	29+95	1.9' LT	7"

AUSTIN RD - HMA PAVEMENT SHALL BE CONSTRUCTED WITH THE FOLLOWING LAYERS AND GRADATIONS:

TYPE	THICKNESS	LAYERS
3 LT 58-28 S	3.00"	LOWER LAYER
5 LT 58-28 S	2.00"	UPPER LAYER

KNOLL VIEW DRIVE - HMA PAVEMENT SHALL BE CONSTRUCTED WITH THE FOLLOWING LAYERS AND GRADATIONS:

TYPE	THICKNESS	LAYERS
3 LT 58-28 S	2.25"	LOWER LAYER
5 LT 58-28 S	1.50"	UPPER LAYER

ABBREVIATIONS

AC	ACRES
AEW	APRON ENDWALL
ASPH	ASPHALT
AVG	AVERAGE
ADT	AVERAGE DAILY TRAFFIC
BAD	BASE AGGREGATE DENSE
BM	BENCHMARK
CL	CENTERLINE OR CLASS
CC	CENTER TO CENTER
CE	COMMERCIAL ENTRANCE
CONC	CONCRETE
CMP	CORRUGATED METAL PIPE
CPRC	CULVERT PIPE CORRUGATED
STEEL	
CSCP	CORRUGATED STEEL CULVERT PIPE
CSM	CERTIFIED SURVEY MAP
CTH	COUNTY TRUNK HIGHWAYS
CULV	CULVERT
CP	CULVERT PIPE
C&G	CURB & GUTTER
D	DEGREE OF CURVATURE
DHV	DESIGN HOURLY VOLUME
DIA	DIAMETER
DWY	DRIVEWAY
E	EAST
ELEV	ELEVATION
EW	ENDWALL
ENT	ENTRANCE
ESALS	EQUIVALENT SINGLE AXLE LOADS
EX	EXISTING
EXC	EXCAVATION
EBS	EXCAVATION BELOW SUBGRADE
EXIST	EXISTING
FF	FACE TO FACE
FERT	FERTILIZER
FE	FEILD ENTRANCE
FG	FINISHED GRADE
FT	FOOT
GV	GAS VALVE
IE	INVERT ELEVATION
INL	INLET
INV	INVERT
IP	IRON PIPE
JCT	JUNCTION
LHF	LEFT HAND FORWARD
L	LENGTH
LS	LUMP SUM
LT	LEFT
MH	MANHOLE
NC	NORMAL CROWN
N	NORTH
PT	POINT
PC	POINT OF CURVATURE
PI	POINT OF INTERSECTION
PT	POINT OF TANGENCY
PL	PROPERTY LINE
PE	PRIVATE ENTRANCE
R/RAD	RADIUS
RCP	REINFORCED CONCRETE PIPE
REQ'D	REQUIRED
RT	RIGHT
R/W	RIGHT-OF-WAY
RHF	RIGHT HAND FORWARD
SALV	SALVAGED
SAN	SANITARY SEWER
SHLDR	SHOULDER
SDD	STANDARD DETAIL DRAWINGS
STA	STATION
STM	STORM SEWER
SE	SUPERELEVATION
SS	STORM SEWER
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
TAN	TANGENT
TLE	TEMPORARY LIMITED EASEMENT
T	TRUCKS
TYP	TYPICAL
VERT	VERTICAL
VC	VERTICAL CURVE
VOL	VOLUME
WV	WATER VALVE
W	WELL
X	EAST GRID COORDINATE
Y	NORTH GRID COORDINATE

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

DNR LIAISON

WISCONSIN DEPARTMENT OF NATURAL RESOURCES
ATTN: LAURA BUB
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
TELEPHONE: (608) 275-3485
EMAIL: LAURA.BUB@WISCONSIN.GOV

DESIGN CONTACT

R.H. BATTERMAN
ATTN: TODD NEEDHAM, P.E.
2857 BARTELLS DRIVE
BELOIT, WI 53511
TELEPHONE: (608) 365-4464
EMAIL: TNEEDHAM@RHBATTERMAN.COM

CITY OF JANESVILLE ENGINEERING

SENIOR ENGINEER
ATTN: LISA WOLF, P.E.
18 N. JACKSON STREET
P.O. BOX 5005
JANESVILLE, WI 53545
TELEPHONE: (608) 755-3162
EMAIL: WOLFL@CI.JANESVILLE.WI.US

ROCK COUNTY HIGHWAY

DIRECTOR OF PUBLIC WORKS
ATTN: BENJAMIN COOPMAN, JR., P.E.
3715 NEWVILLE ROAD
JANESVILLE, WI 53545
TELEPHONE: (608) 757-5450
EMAIL: COOPMAN@CO.ROCK.WI.US

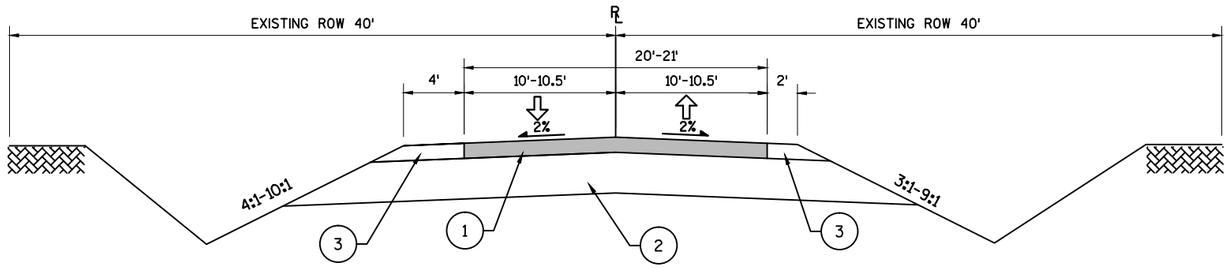
TOWN OF JANESVILLE

TOWN CHAIR
ATTN: BRUCE SCHNEIDER
1628 N. LITTLE COURT
JANESVILLE, WI 53548
TELEPHONE: (608) 751-1245

ROCK COUNTY SURVEYOR

ATTN: BRAD HEUER, PLS
ROCK COUNTY LAND SURVEYOR
51 SOUTH MAIN STREET
JANESVILLE, WI 53545
TELEPHONE: (608) 757-5658
EMAIL: BRAD.HEUER@CO.ROCK.WI.US

2

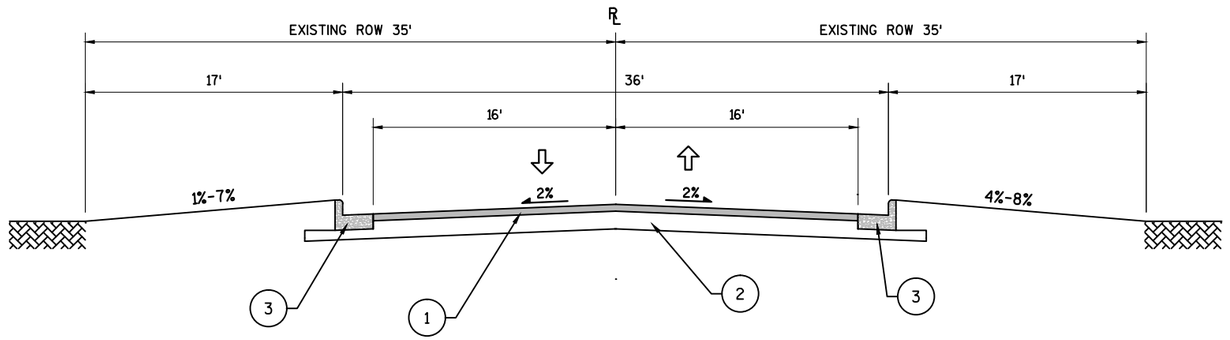


LEGEND

(1) 5"-10" ASPHALTIC PAVEMENT
(2) 3"-5" BASE AGGREGATE
(3) BASE AGGREGATE (SHLD.)

TYPICAL EXISTING SECTION

AUSTIN ROAD
STA 5+10 - 31+50



LEGEND

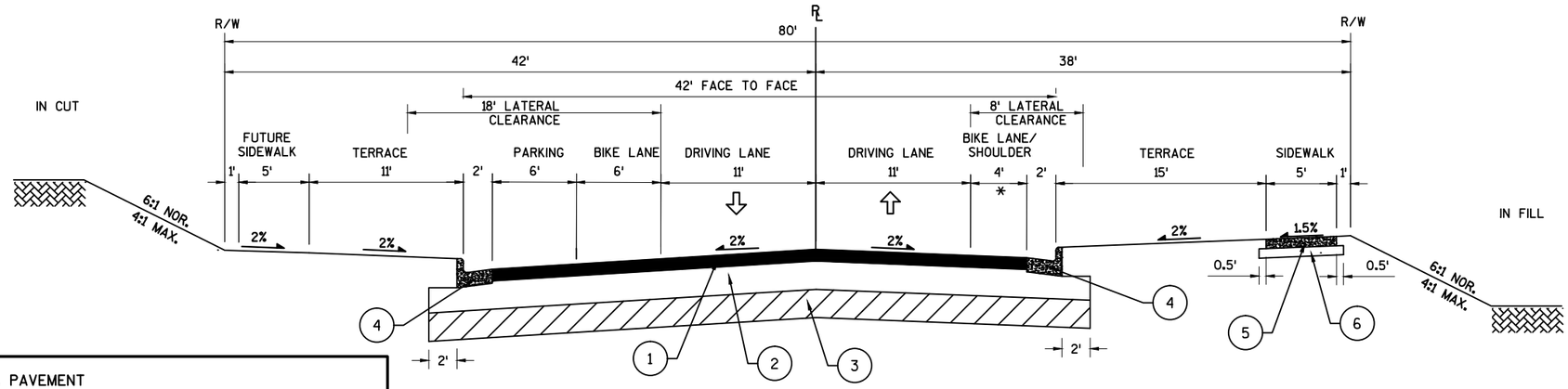
(1) 2.75" ASPHALTIC PAVEMENT
(2) 7" BASE AGGREGATE
(3) 30" CONCRETE CURB & GUTTER

TYPICAL EXISTING SECTION

KNOLL VIEW DRIVE
STA 100+00 - 101+50

PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	EXISTING TYPICAL SECTIONS	SHEET 3 E

2

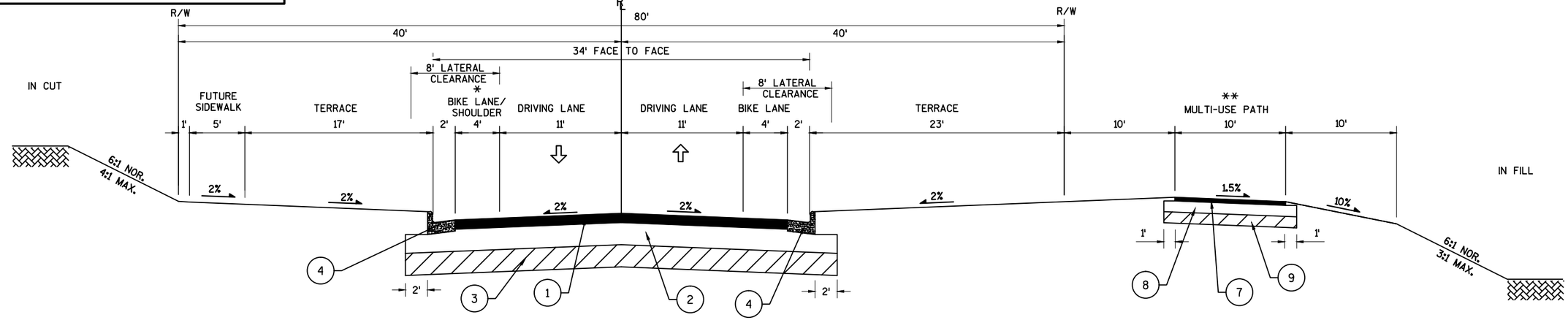


LEGEND

1. 5 INCH HMA PAVEMENT
   2-INCHES HMA PAVEMENT 5 LT 58-28 S (UPPER LAYER)
   3-INCHES HMA PAVEMENT 3 LT 58-28 S (LOWER LAYER)
2. 12 INCH BASE AGGREGATE DENSE 1 ¼ INCH
3. 12 INCH SELECT CRUSHED MATERIAL
4. CONCRETE CURB & GUTTER 30 INCH TYPE L
5. CONCRETE SIDEWALK 4 INCH OR CONCRETE DRIVEWAY 6 INCH
6. 4 INCH BASE AGGREGATE DENSE 1 ¼ INCH
7. 3 INCH ASPHALTIC SURFACE
8. 6 INCH BASE AGGREGATE DENSE 1 ¼ INCH
9. 6 INCH SELECT CRUSHED MATERIAL

TYPICAL PROPOSED SECTION

AUSTIN ROAD NORTH
STA 15+17.68 - 31+50
*STA 29+00 - 31+50
VARIES 4'-15'



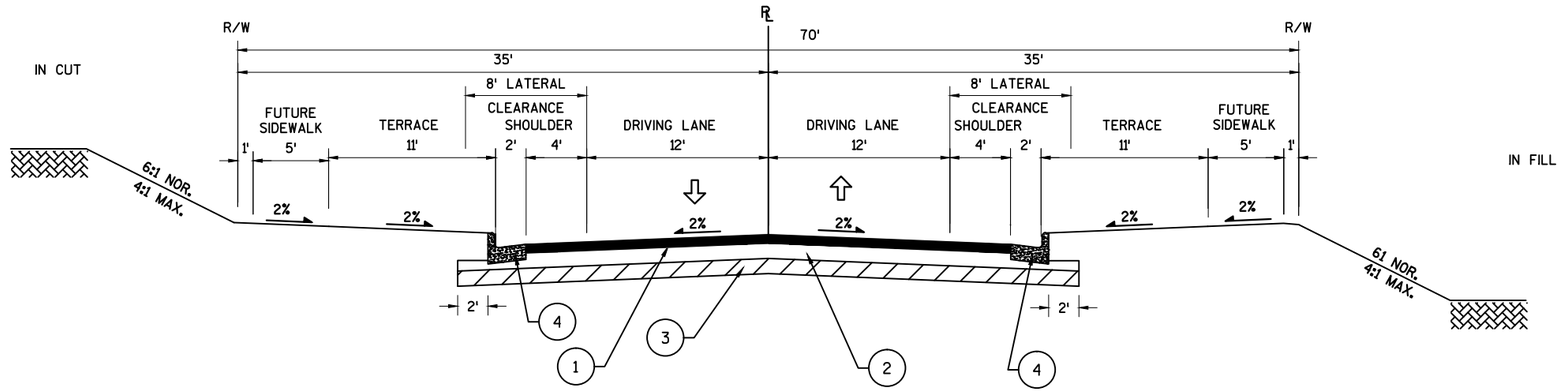
TYPICAL PROPOSED SECTION

AUSTIN ROAD SOUTH
STA 5+10 - 15+17.68
* STA 14+18 - 15+18
VARIES 4' - 12'
** STA 14+20 - 15+28
VARIES 5' - 10'

PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	PROPOSED TYPICAL SECTIONS	SHEET 4 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_020301_TS.DWG
LAYOUT NAME - 59900036_020301_TS - PROPOSED AUSTIN RD REVISED (3)
PLOT DATE : 11/1/2016 3:30 PM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

2



TYPICAL PROPOSED SECTION

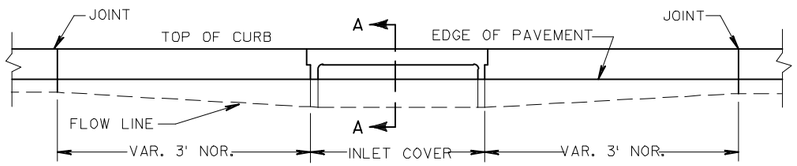
KNOLL VIEW DRIVE
STA 100+00 - 101+50

LEGEND

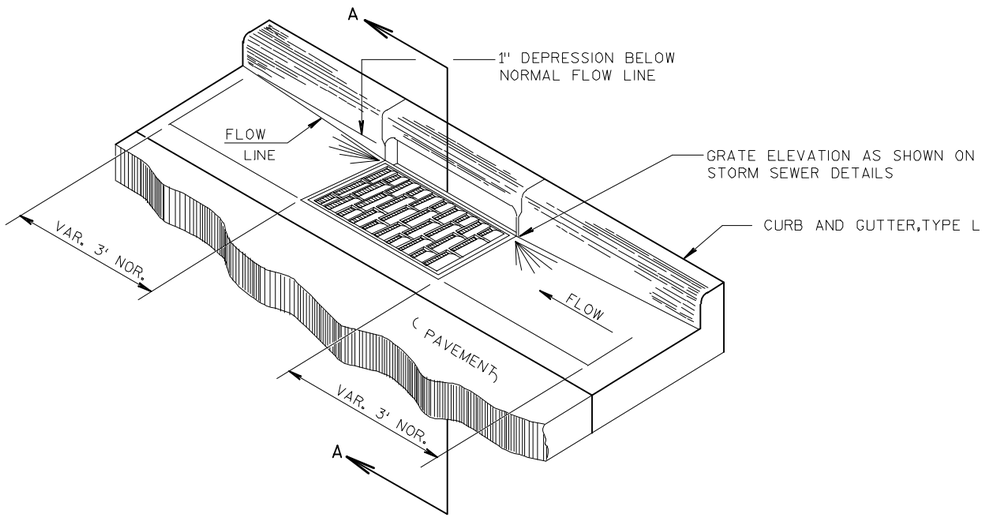
1. 3.75 INCH HMA PAVEMENT
   1 1/2-INCHES HMA PAVEMENT 5 LT 58-28 S (UPPER LAYER)
   2 1/4-INCHES HMA PAVEMENT 3 LT 58-28 S (LOWER LAYER)
2. 6 INCH BASE AGGREGATE DENSE 1 ¼ INCH
3. 6 INCH SELECT CRUSHED MATERIAL
4. CONCRETE CURB & GUTTER 30 INCH TYPE L

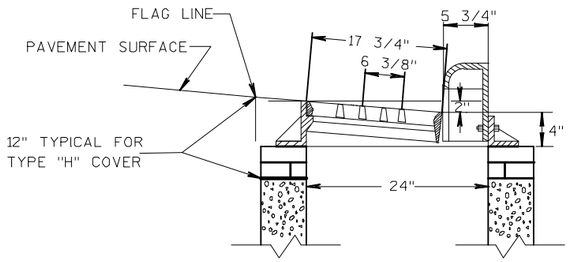
PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	PROPOSED TYPICAL SECTION	SHEET 5	E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_020301_TS.DWG
LAYOUT NAME - 59900036_020301_TS - PROPOSED KNOLL VIEW DR REVISED
PLOT DATE : 7/21/2016 3:30 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42



ELEVATION

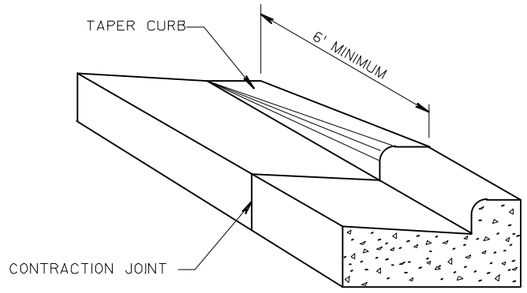




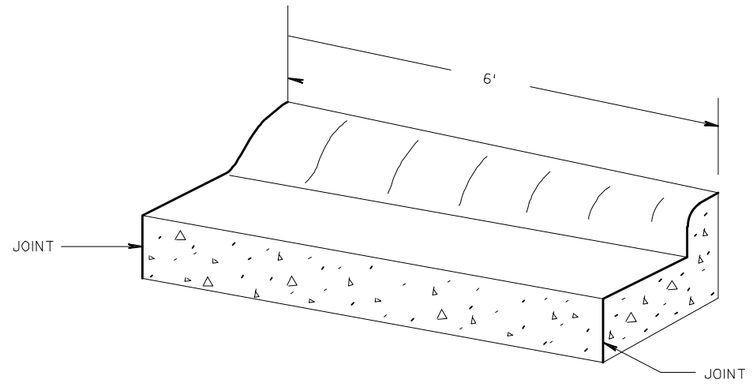
SECTION A-A

DETAIL OF CURB AND GUTTER AT INLETS

(TYPE 3-H INLET SHOWN)

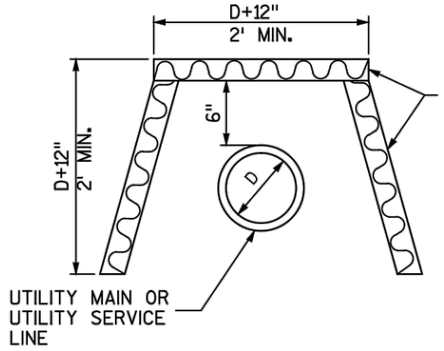


DETAIL OF CURB & GUTTER TERMINI

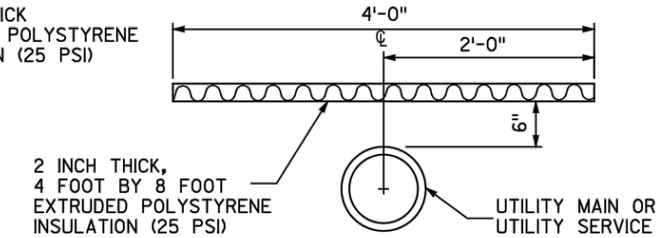


TRANSITION DETAIL

36" TYPE "D" CURB & GUTTER TO 30" TYPE "L" CURB & GUTTER
(TO BE MEASURED & PAID FOR AS 36" CONC. C&G)



SIDE PROTECTION INSTALLATION

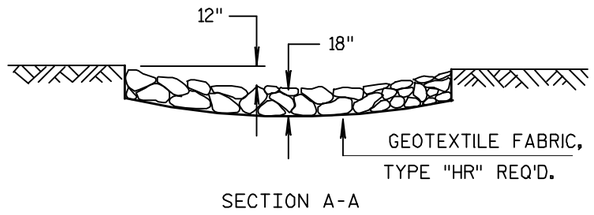


STANDARD INSTALLATION

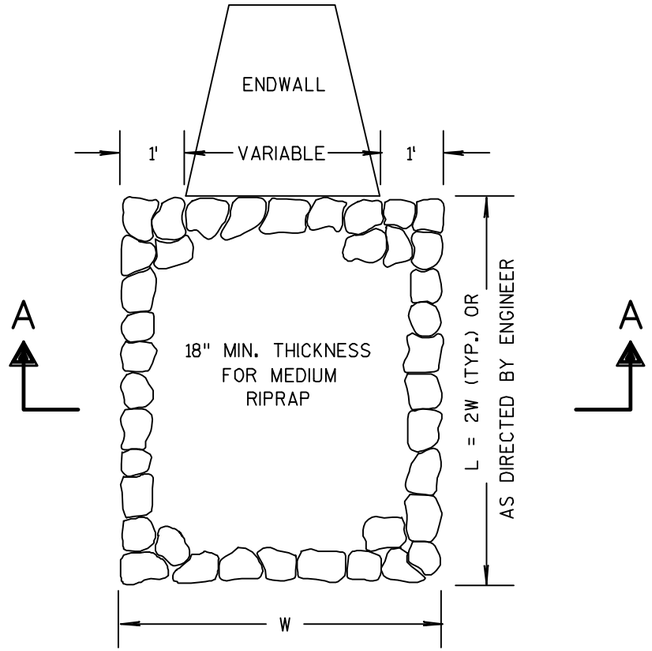
NOTE: THE SIDE PROTECTION INSULATION SHALL BE USED WHERE FROST WILL PENETRATE BELOW THE PIPE INVERT

PIPE INSULATION DETAIL

SCALE: NONE



SECTION A-A

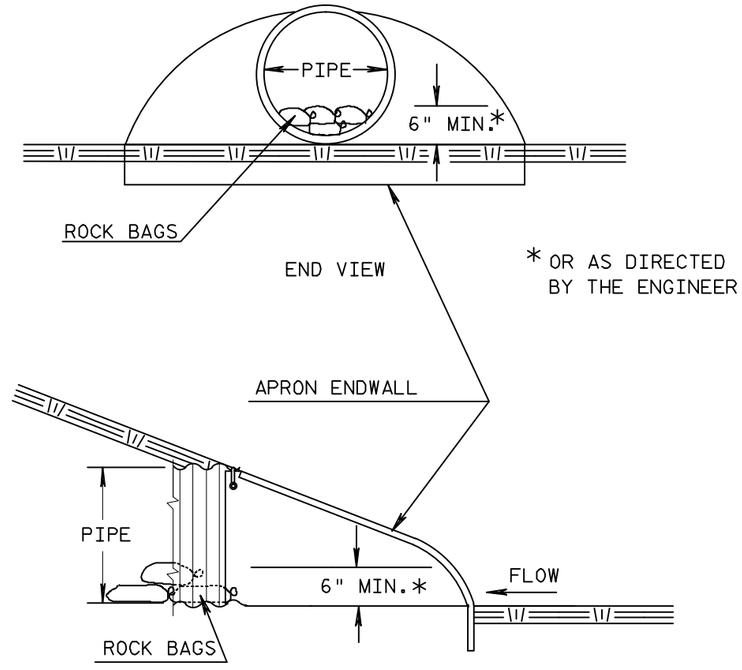


MEDIUM RIPRAP TREATMENT AT CULVERTS

RUNOFF COEFFICIENT TABLE

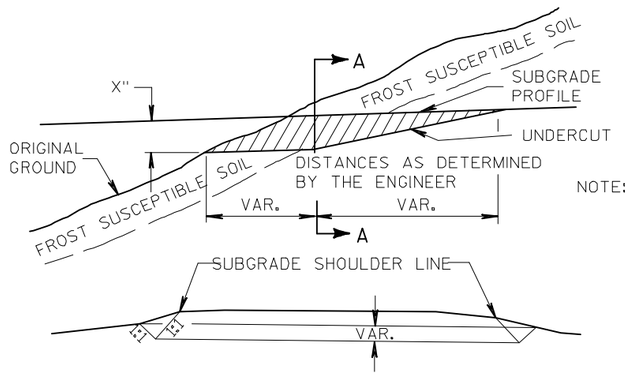
	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = _____ ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = _____ACRES



SIDE VIEW

CULVERT PIPE CHECKS



SECTION A-A

CROSS SECTION SHOWING UNDERCUT

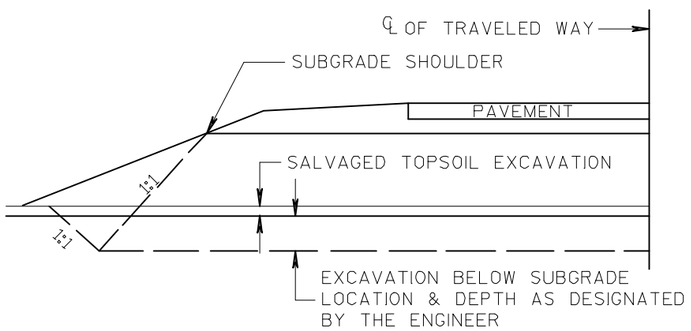
DETAIL FOR EXCAVATION BELOW SUBGRADE AT CUTS

NOTE: EXACT LOCATIONS AND EXTENT OF E.B.S. SECTIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

E.B.S. AREA TO BE BACKFILLED WITH MATERIAL ACCEPTABLE TO THE ENGINEER

BACKFILL MUST BE HOMOGENEOUS WITH ADJOINING FILL MATERIAL.

THE FILL SECTION WITHIN 100' OF THE MOUTH OF THE CUT MUST BE KEPT 2' BELOW SUBGRADE UNTIL E.B.S. IS COMPLETED.



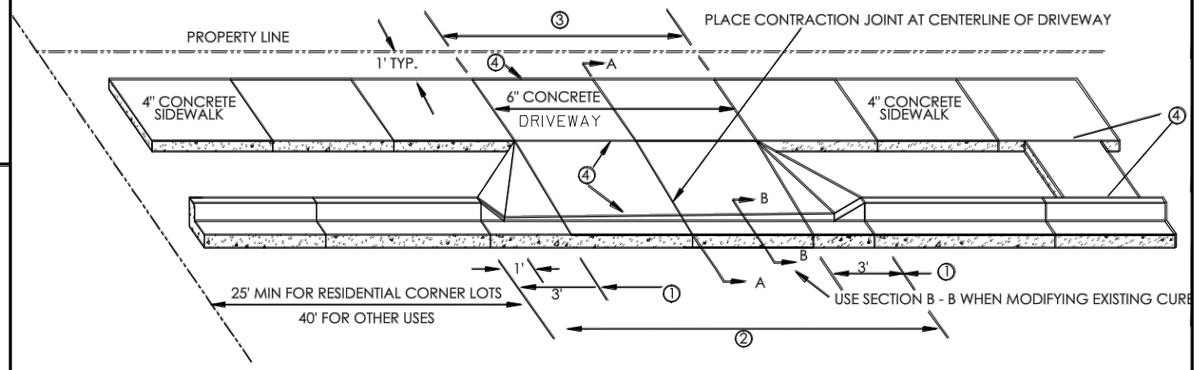
DETAIL FOR EXCAVATION BELOW SUBGRADE

City of Janesville
Engineering Division
January - 2013
last revision: January 2016

DRIVEWAY APPROACH

DETAIL # 19

NOT TO SCALE



THESE STANDARDS APPLY FOR SINGLE OR DOUBLE DRIVEWAYS. DOUBLE DRIVEWAYS SHALL HAVE AT LEAST 10' ON EACH PROPERTY. APPROACHES SHALL BE 6" CONCRETE OR 2" ASPHALT (MIN). THE SITE PLAN REVIEW COORDINATOR AND CITY ENGINEER MAY APPROVE LARGER OPENINGS FOR SPECIAL CONDITIONS.

REMOVE AND REPLACE THE ENTIRE CURB AND GUTTER THROUGH THE DRIVEWAY OPENING OR PROFILE SAW CUT AND REMOVE THE CURB HEAD USING A CONCRETE SAW DESIGNED FOR THIS PURPOSE.

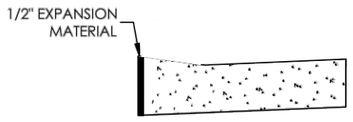
① DRIVEWAY FLARE ON EACH SIDE SHALL EQUAL 0.6 TIMES THE DISTANCE BETWEEN THE WALK AND THE BACK OF THE CURB, BUT NOT TO EXCEED 3 FEET.

② 30' MAX RESID. 1 OR 2 CAR GARAGE
36' MAX RESID. 3 CAR GARAGE AND OTHER THAN RESID. (BUILDING OFFICIAL APPROVAL REQUIRED)
42' MAX DUPLEX 2 CAR SIDE-BY-SIDE (BUILDING OFFICIAL APPROVAL REQUIRED)

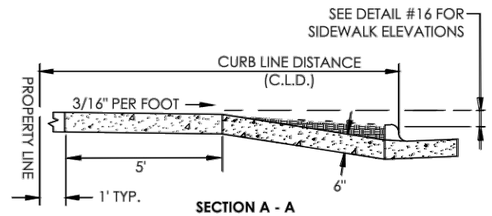
③ 24' MAX RESID. 1 OR 2 CAR GARAGE
30' MAX RESID. 3 CAR GARAGE AND OTHER THAN RESID. (BUILDING OFFICIAL APPROVAL REQUIRED)
36' MAX DUPLEX 2 CAR SIDE-BY-SIDE (BUILDING OFFICIAL APPROVAL REQUIRED)

④ EXPANSION JOINT MATERIAL

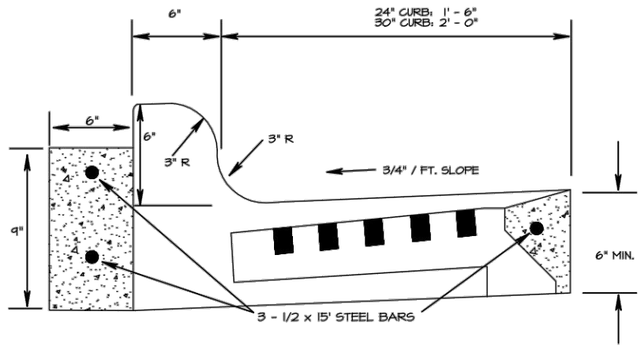
NOTE: CURB OPENING PERMIT REQUIRED (CONTACT BUILDING AND DEVELOPMENT SERVICES DIVISION)



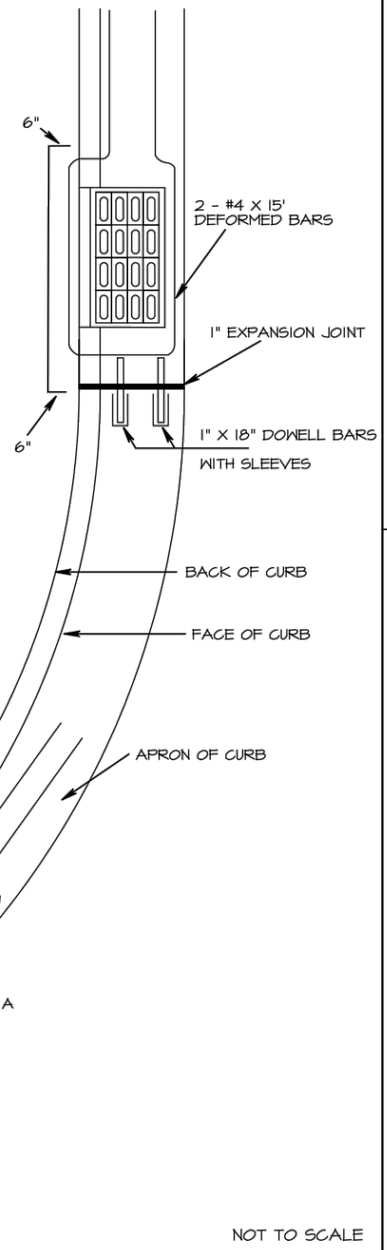
SECTION B - B



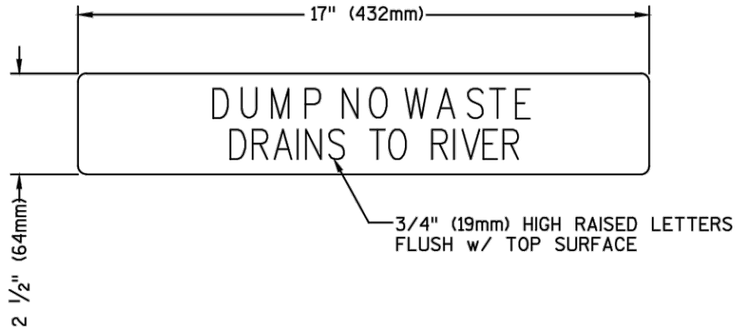
SECTION A - A



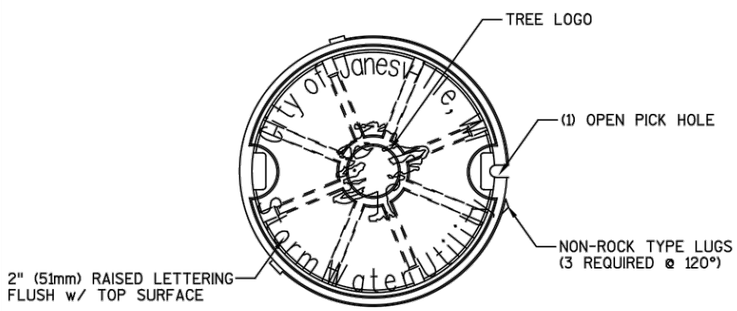
SECTION A - A

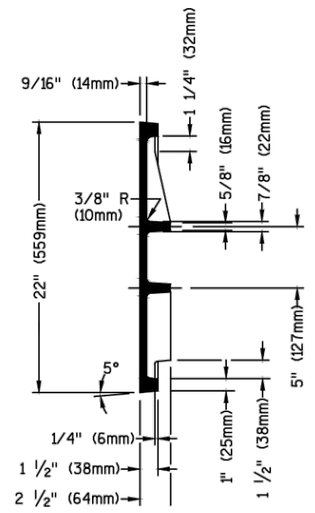


NOT TO SCALE



INLET COVER SPECIAL
SCALE: NONE





NOTE: ALL DIMENSIONS SHOWN ARE IN ENGLISH AND [METRIC]
MATERIAL: CAST GRAY IRON ASTM A-48, CLASS 35B
FINISH: NO PAINT
WEIGHT: 95#

MANHOLE COVER SPECIAL
SCALE: NONE

CITY OF JANESVILLE
ENGINEERING DEPT.
JANUARY - 2013
LAST REVISION: 1/13

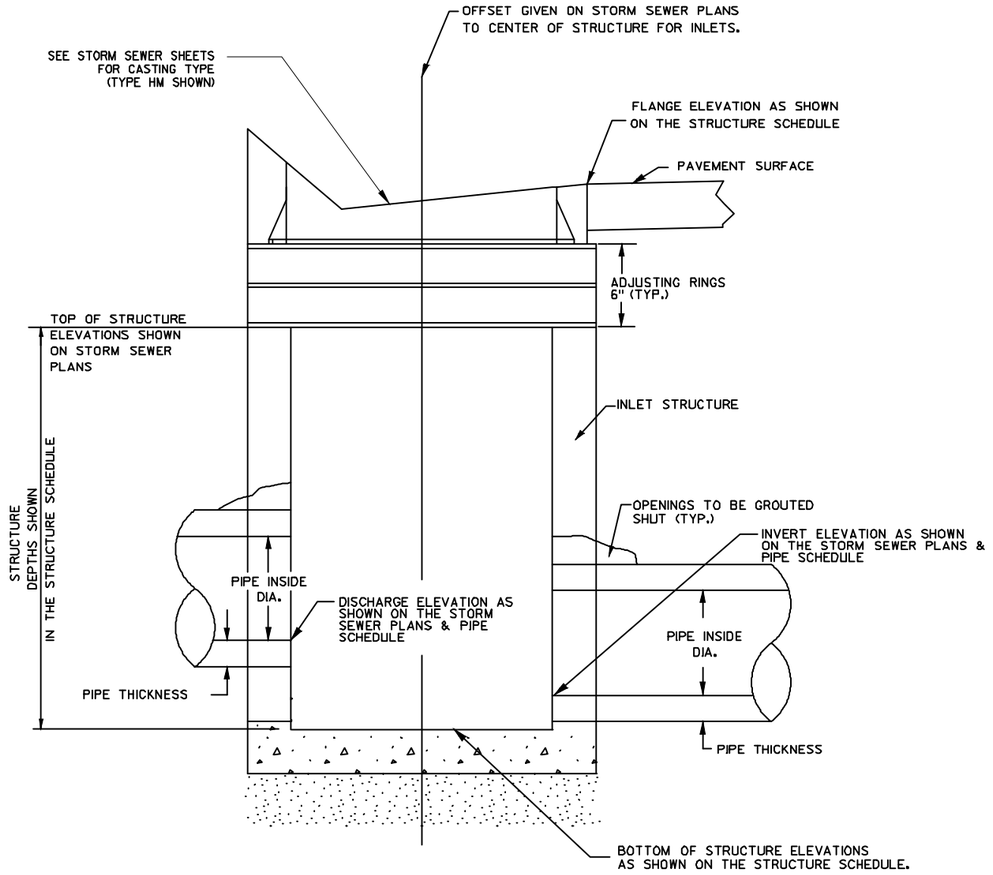
CURB AND STEEL REINFORCEMENT AT INLETS

DETAIL # 15

GENERAL NOTES:

GRANULAR BACKFILL REQUIRED AROUND INLET
(INCIDENTAL TO CONSTRUCTION OF INLET)

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, APPLICABLE SPECIAL PROVISIONS, S.D.D. FOR INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT.

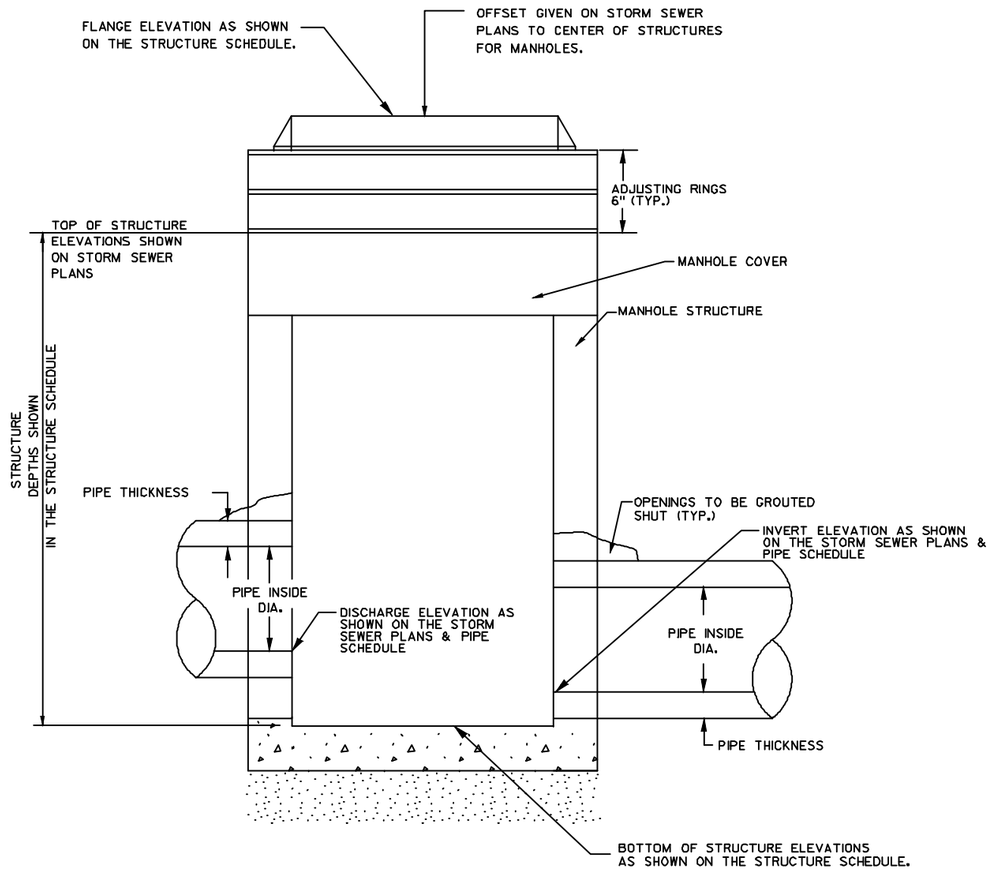


DETAIL OF INLET W/CASTING

GENERAL NOTES:

GRANULAR BACKFILL REQUIRED AROUND MANHOLE(INCIDENTAL TO CONSTRUCTION OF MANHOLE)

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, APPLICABLE SPECIAL PROVISIONS, S.D.D. FOR MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER.



DETAIL OF MANHOLE W/CASTING

2

AUSTIN ROAD

7+00

6+00

5+00

BP: 4+50.00

R/W

MULTI-USE PATH

TLE

P/L

U.E.

U.E.

U.E.

RE-GRADE
EXISTING DITCH

837
836
835
834
838
839
840

WEST COURT STREET

MH

GUY

SAN

OH

N

PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CONSTRUCTION DETAILS - COURT ST. DRAINAGE SWALE	SHEET 11 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_021001_CD.DWG
LAYOUT NAME - CD

PLOT DATE : 11/2/2016 3:18 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:40 FT

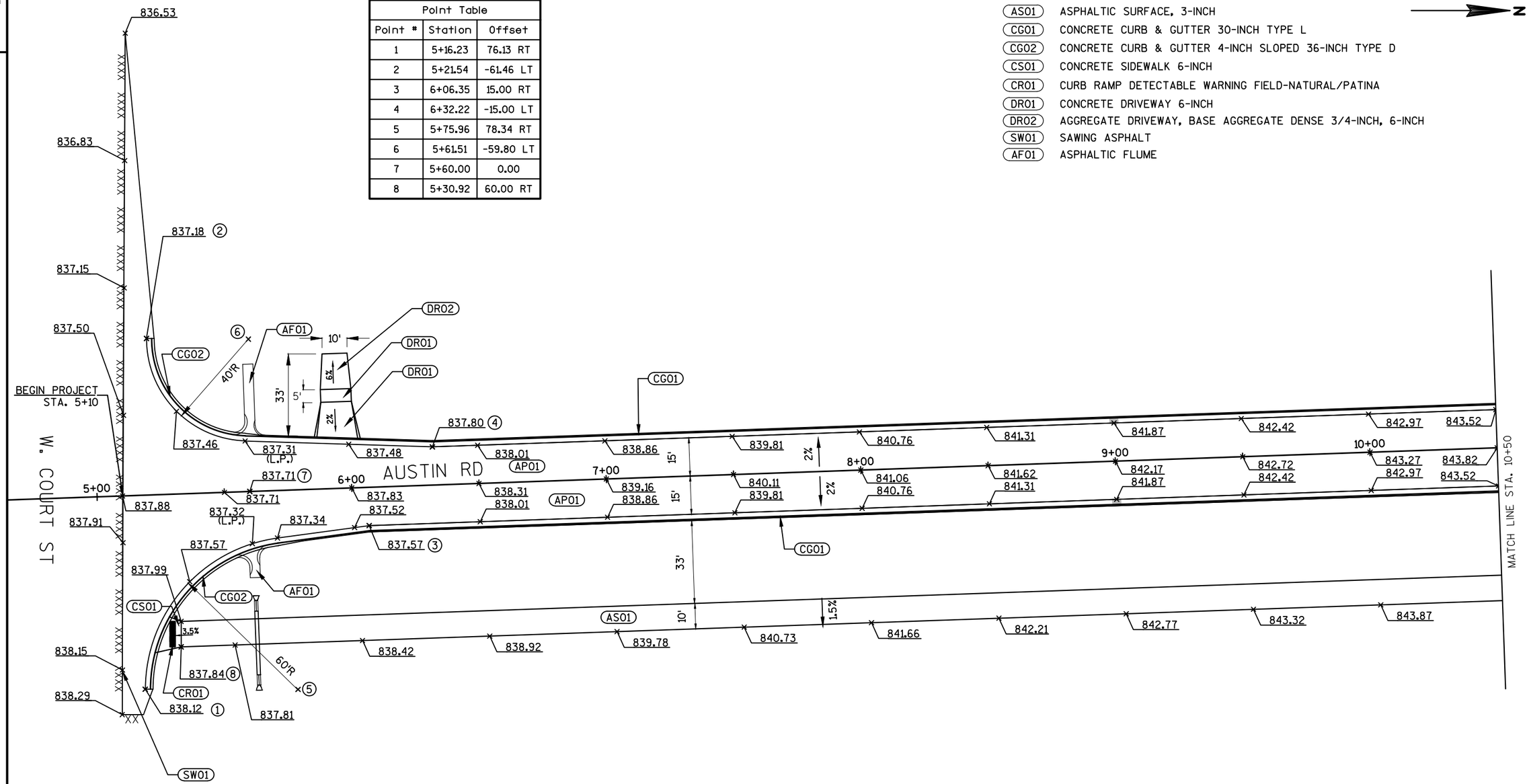
WISDOT/CADDS SHEET 42

2

Point Table		
Point #	Station	Offset
1	5+16.23	76.13 RT
2	5+21.54	-61.46 LT
3	6+06.35	15.00 RT
4	6+32.22	-15.00 LT
5	5+75.96	78.34 RT
6	5+61.51	-59.80 LT
7	5+60.00	0.00
8	5+30.92	60.00 RT

LEGEND

- (AP01) HMA PAVEMENT, 5-INCH
- (AS01) ASPHALTIC SURFACE, 3-INCH
- (CG01) CONCRETE CURB & GUTTER 30-INCH TYPE L
- (CG02) CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D
- (CS01) CONCRETE SIDEWALK 6-INCH
- (CR01) CURB RAMP DETECTABLE WARNING FIELD-NATURAL/PATINA
- (DR01) CONCRETE DRIVEWAY 6-INCH
- (DR02) AGGREGATE DRIVEWAY, BASE AGGREGATE DENSE 3/4-INCH, 6-INCH
- (SW01) SAWING ASPHALT
- (AF01) ASPHALTIC FLUME



PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	PLAN DETAILS	SHEET 12 E

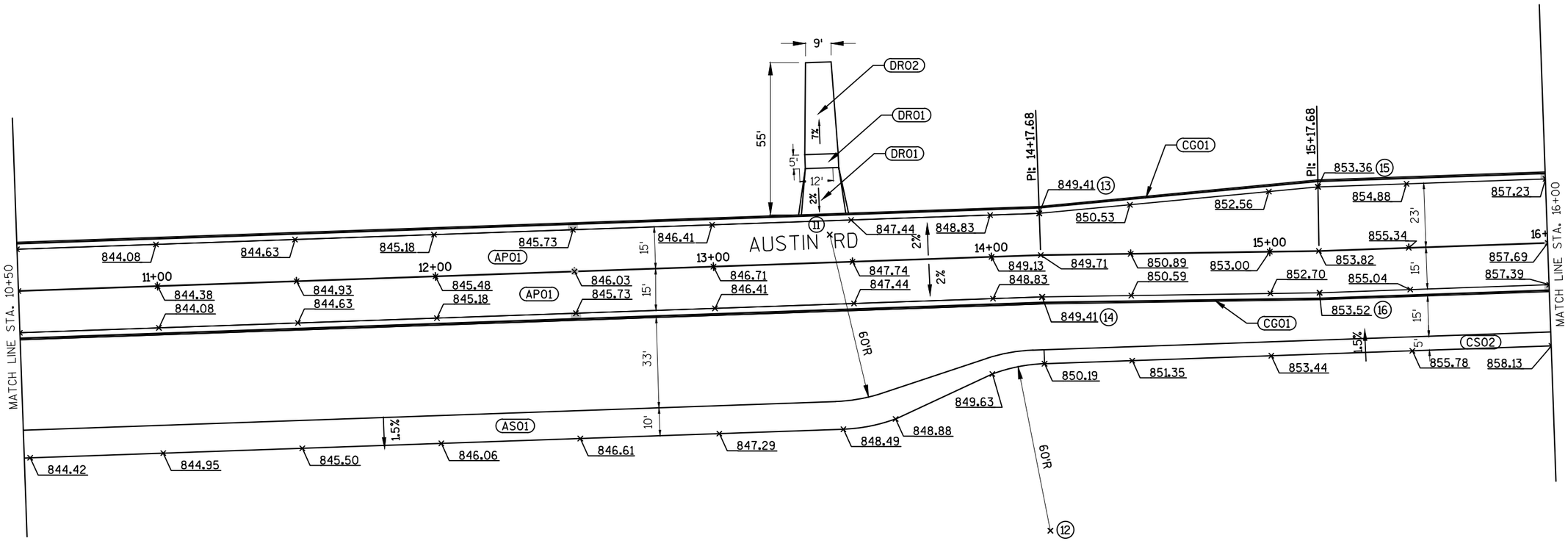
FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_021201_PD.DWG
LAYOUT NAME - 59900036_021201_PD - PD 1
PLOT DATE : 11/1/2016 3:54 PM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

Point Table		
Point #	Station	Offset
11	13+42.13	-10.00 LT
12	14+19.67	98.98 RT
13	14+17.68	-15.00 LT
14	14+17.98	15.00 RT
15	15+18.14	-23.00 LT
16	15+17.68	15.00 RT

LEGEND

- (AP01) HMA PAVEMENT, 5-INCH
- (AS01) ASPHALTIC SURFACE, 3-INCH
- (CG01) CONCRETE CURB & GUTTER 30-INCH TYPE L
- (CS01) CONCRETE SIDEWALK 6-INCH
- (CS02) CONCRETE SIDEWALK 4-INCH
- (DR01) CONCRETE DRIVEWAY 6-INCH
- (DR02) AGGREGATE DRIVEWAY, BASE AGGREGATE DENSE 3/4-INCH, 6-INCH

N



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN DETAILS	SHEET 13 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_021201_PD.DWG
LAYOUT NAME - 59900036_021201_PD - PD 2

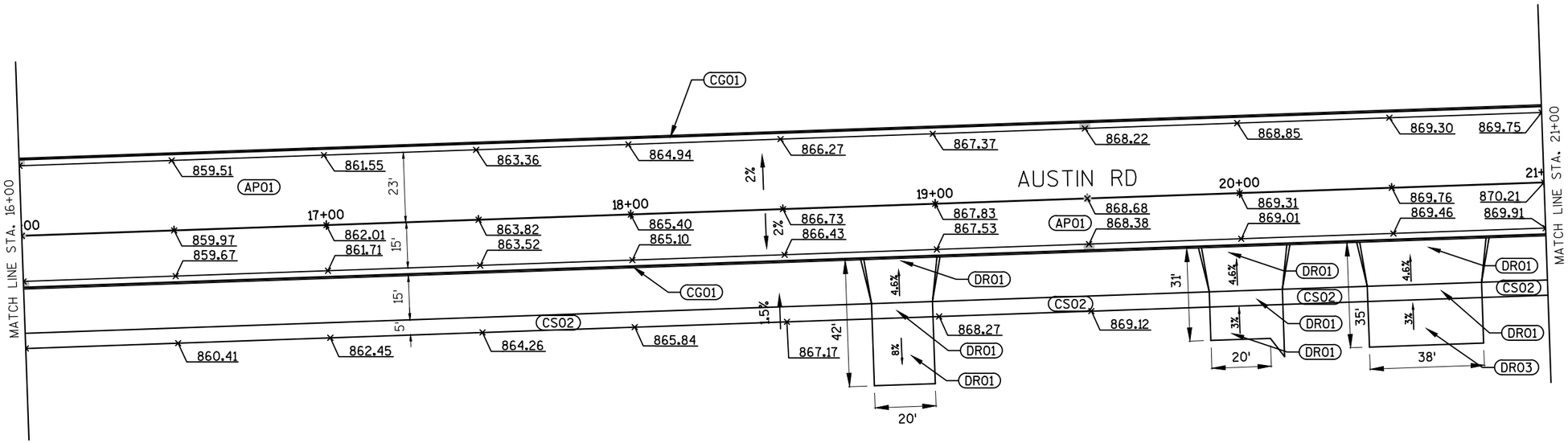
PLOT DATE : 11/1/2016 3:54 PM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : 1 IN:40 FT

2

LEGEND

AP01	HMA PAVEMENT, 5-INCH
CG01	CONCRETE CURB & GUTTER 30-INCH TYPE L
CS01	CONCRETE SIDEWALK 6-INCH
CS02	CONCRETE SIDEWALK 4-INCH
DR01	CONCRETE DRIVEWAY 6-INCH
DR03	ASPHALTIC SURFACE DRIVEWAY & FIELD ENTRANCES, 3-INCH

N



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN DETAILS	SHEET 14 E

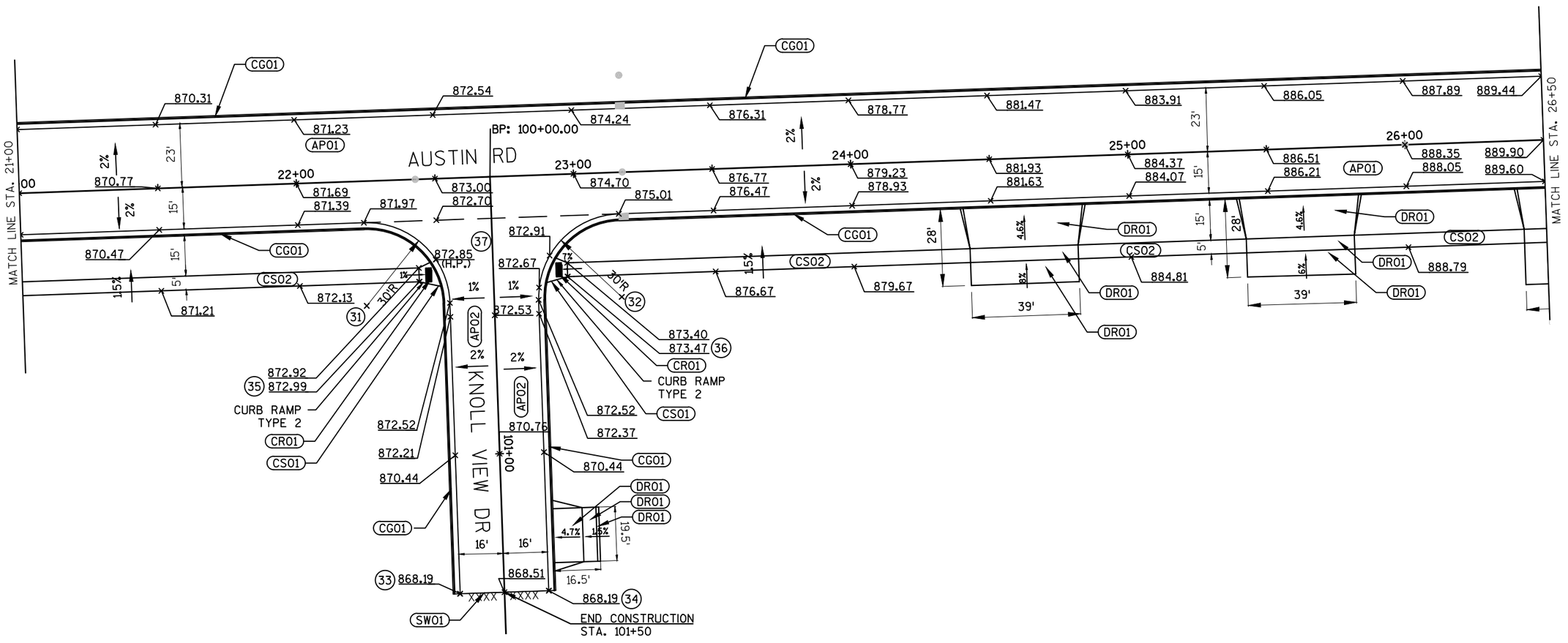
FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_021201_PD.DWG
LAYOUT NAME - 59900036_021201_PD - PD 3

PLOT DATE : 11/1/2016 3:54 PM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : 1 IN:40 FT

Point Table		
Point #	Station	Offset
31	22+23.83	45.00 RT
32	23+15.83	45.00 RT
33	101+50.00	16.00 RT
34	101+50.00	-16.00 LT
35	22+43.07	37.00 RT
36	22+96.60	37.00 RT
37	100+29.17	20.52 RT

LEGEND

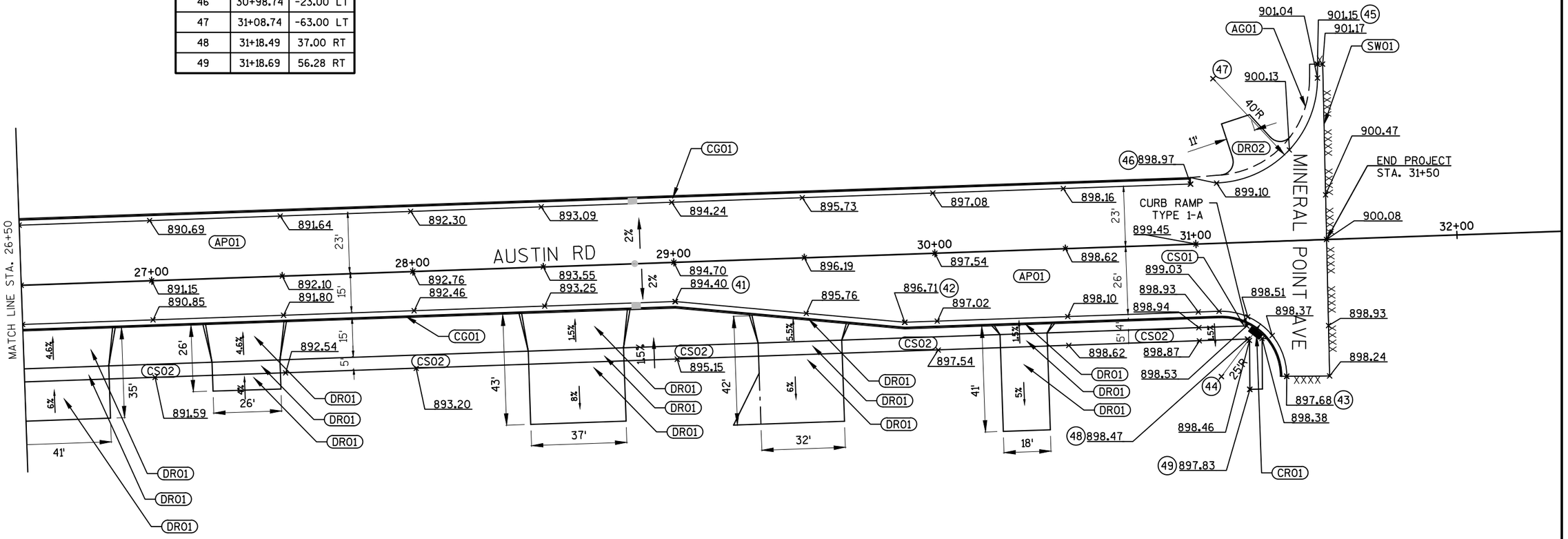
AP01	HMA PAVEMENT, 5-INCH
AP02	HMA PAVEMENT TYPE E-1, 3.75-INCH
CG01	CONCRETE CURB & GUTTER 30-INCH TYPE L
CS01	CONCRETE SIDEWALK 6-INCH
CS02	CONCRETE SIDEWALK 4-INCH
DR01	CONCRETE DRIVEWAY 6-INCH
CR01	CURB RAMP DETECTABLE WARNING FIELD-NATURAL/PATINA
SW01	SAWING ASPHALT



Point Table		
Point #	Station	Offset
41	28+99.96	15.00 RT
42	29+87.55	26.00 RT
43	31+32.98	51.76 RT
44	31+07.99	51.00 RT
45	31+48.88	-67.14 LT
46	30+98.74	-23.00 LT
47	31+08.74	-63.00 LT
48	31+18.49	37.00 RT
49	31+18.69	56.28 RT

LEGEND

- (AG01) BASE AGGREGATE DENSE, 3/4-INCH
- (AP01) HMA PAVEMENT, 5-INCH
- (CG01) CONCRETE CURB & GUTTER 30-INCH TYPE L
- (CS01) CONCRETE SIDEWALK 6-INCH
- (CS02) CONCRETE SIDEWALK 4-INCH
- (DR01) CONCRETE DRIVEWAY 6-INCH
- (DR02) AGGREGATE DRIVEWAY, BASE AGGREGATE DENSE 3/4-INCH, 6-INCH
- (CR01) CURB RAMP DETECTABLE WARNING FIELD-NATURAL/PATINA
- (SW01) SAWING ASPHALT



2

N

R/W

INSTALL TRACKING PAD

P.L.

P.L.

EXISTING WETLAND

TLE

TLE

R/W

R/W

W. COURT ST

AUSTIN RD

5+00

6+00

7+00

8+00

9+00

10+00

MATCH LINE STA. 10+50

R/W

TLE

TLE

R/W

P.L.

P.L.

LEGEND

EROSION MAT URBAN CLASS II, TYPE A

SILT FENCE

RIP RAP

SLOPE INTERCEPT

INLET PROTECTION TYPE (X)

SURFACE WATER FLOW

CULVERT PIPE CHECK

EROSION CONTROL GENERAL NOTES

1. AREAS WITHIN THE SLOPE INTERCEPTS NOT USING EROSION MAT NEED TO HAVE TOPSOIL, FERTILIZER, SEED, AND MULCH PLACED.

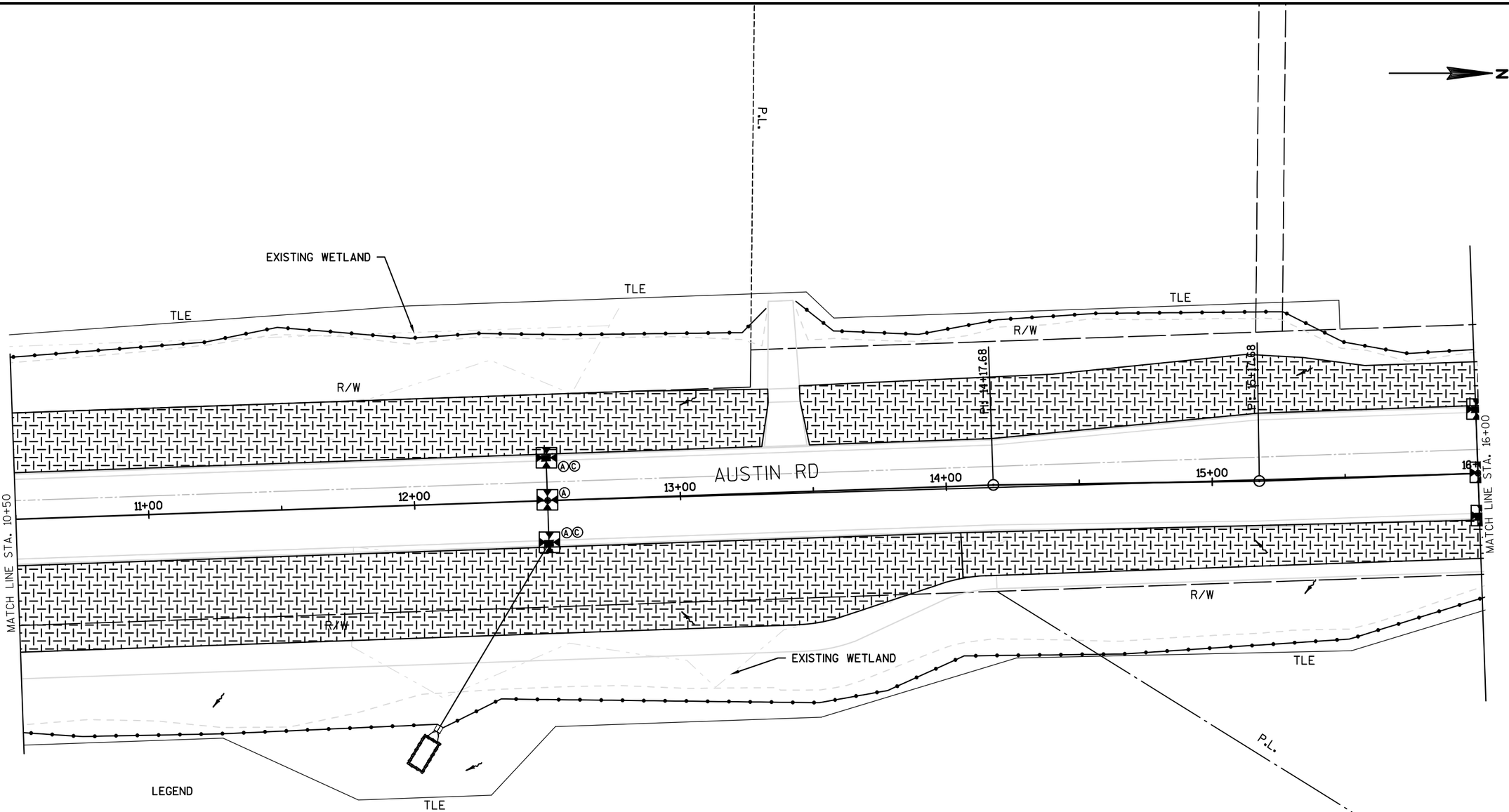
2. WHEN INLET PROTECTION TYPE A AND TYPE B, C, OR D ARE INDICATED AT THE SAME LOCATION, USE TYPE A PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER AND USE TYPE B, C, OR D AFTER THE INLET CASTINGS ARE IN PLACE.

3. RESTORATION OF EXPOSED SLOPES AND DITCHES SHALL TAKE PLACE NOT MORE THAN 7 DAYS AFTER FINISHED GRADING IS COMPLETE.

PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	EROSION CONTROL	SHEET 17 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_022001_EC.DWG
LAYOUT NAME - 59900036_022001_EC - EC 1
PLOT DATE : 7/21/2016 3:50 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

2



LEGEND

- EROSION MAT URBAN CLASS I, TYPE A
- SILT FENCE
- RIP RAP
- SLOPE INTERCEPT
- INLET PROTECTION TYPE (X)
- SURFACE WATER FLOW
- CULVERT PIPE CHECK

EROSION CONTROL GENERAL NOTES

1. AREAS WITHIN THE SLOPE INTERCEPTS NOT USING EROSION MAT NEED TO HAVE TOPSOIL, FERTILIZER, SEED, AND MULCH PLACED.

2. WHEN INLET PROTECTION TYPE A AND TYPE B, C, OR D ARE INDICATED AT THE SAME LOCATION, USE TYPE A PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER AND USE TYPE B, C, OR D AFTER THE INLET CASTINGS ARE IN PLACE.

3. RESTORATION OF EXPOSED SLOPES AND DITCHES SHALL TAKE PLACE NOT MORE THAN 7 DAYS AFTER FINISHED GRADING IS COMPLETE.

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	EROSION CONTROL	SHEET 18 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_022001_EC.DWG
LAYOUT NAME - 59900036_022001_EC - EC 2
PLOT DATE : 7/21/2016 3:50 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

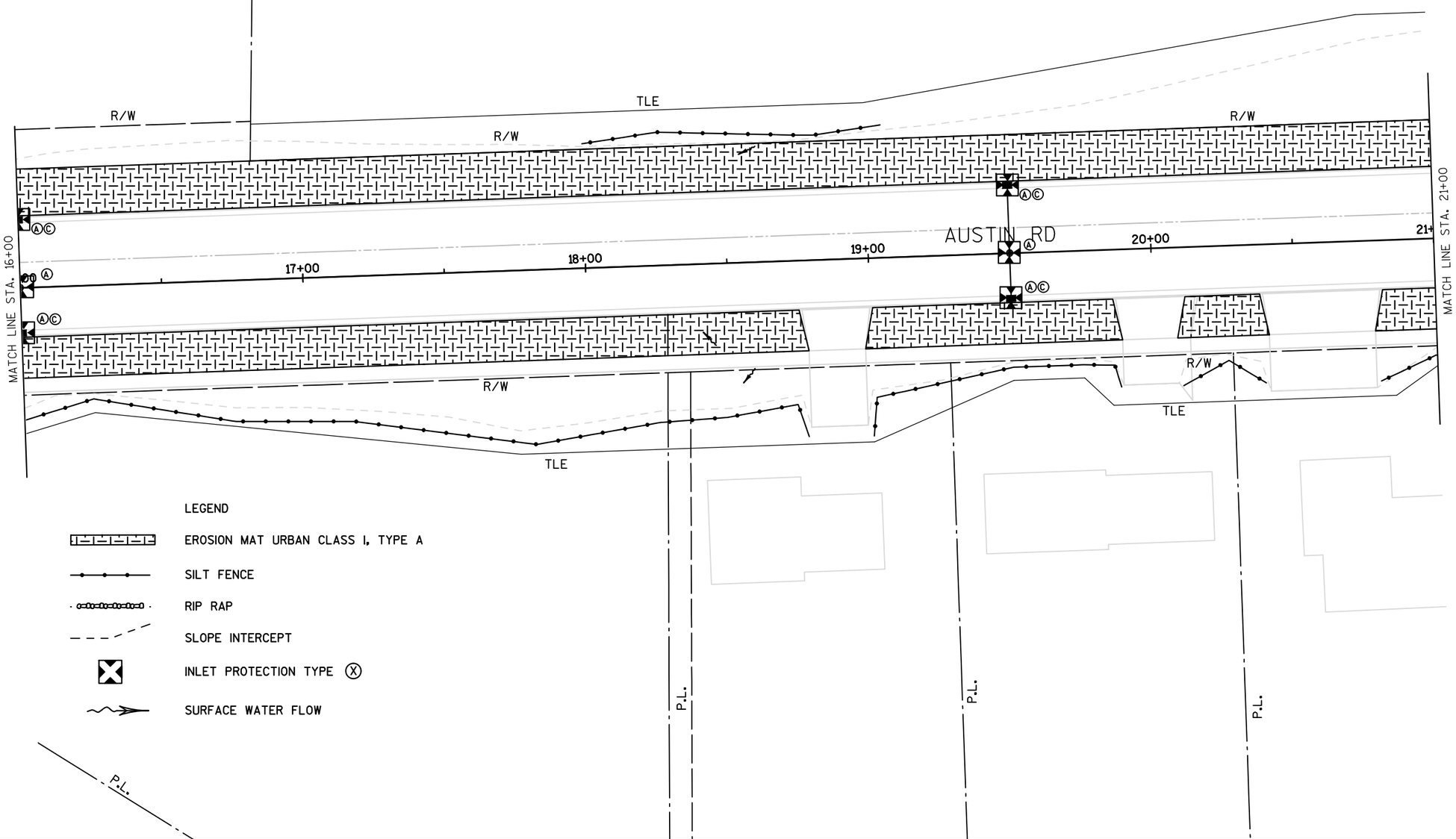
2

EROSION CONTROL GENERAL NOTES

1. AREAS WITHIN THE SLOPE INTERCEPTS NOT USING EROSION MAT NEED TO HAVE TOPSOIL, FERTILIZER, SEED, AND MULCH PLACED.

2. WHEN INLET PROTECTION TYPE A AND TYPE B, C, OR D ARE INDICATED AT THE SAME LOCATION, USE TYPE A PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER AND USE TYPE B, C, OR D AFTER THE INLET CASTINGS ARE IN PLACE.

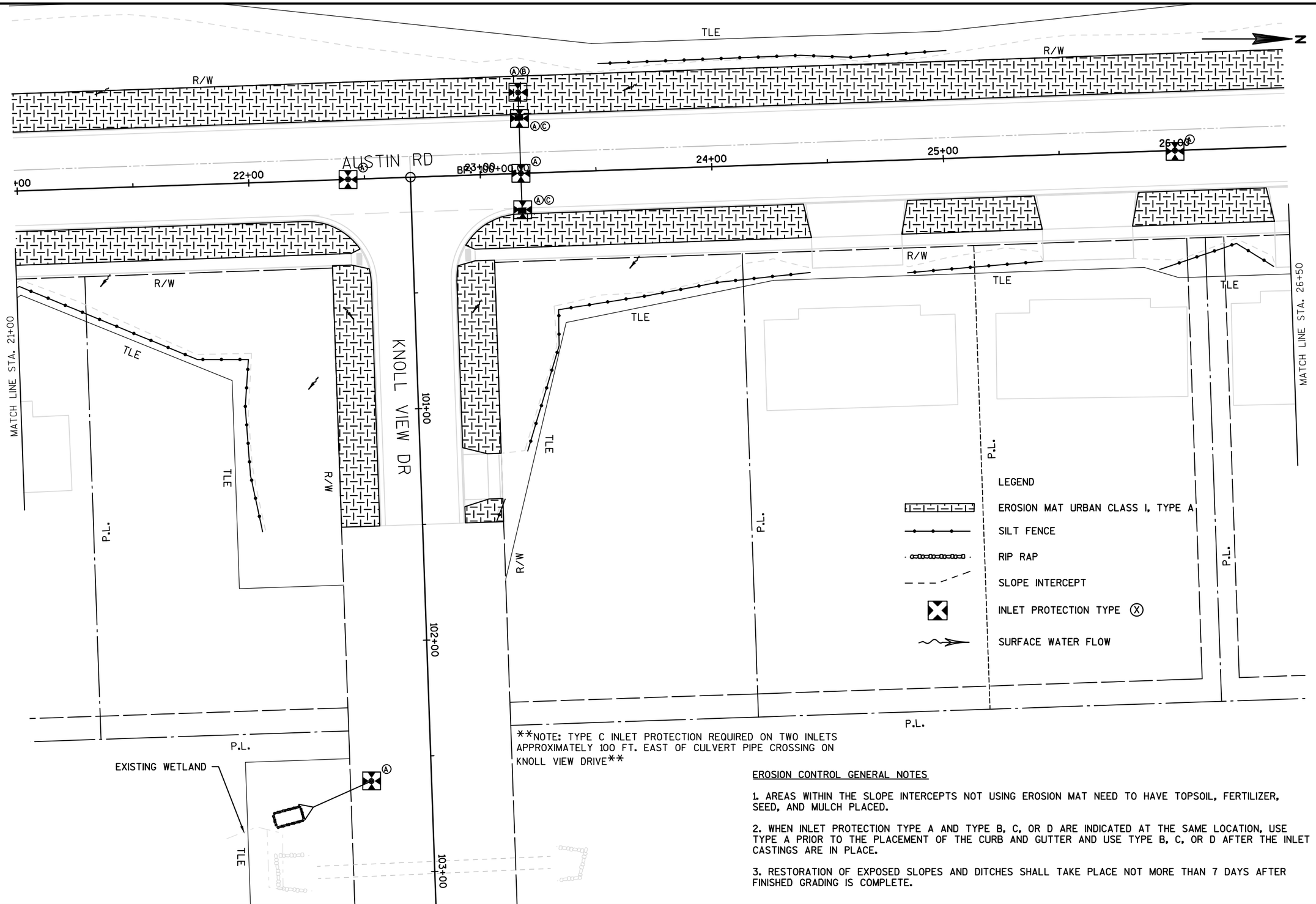
3. RESTORATION OF EXPOSED SLOPES AND DITCHES SHALL TAKE PLACE NOT MORE THAN 7 DAYS AFTER FINISHED GRADING IS COMPLETE.



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	EROSION CONTROL	SHEET 19 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_022001_EC.DWG
LAYOUT NAME - 59900036_022001_EC - EC 3
PLOT DATE : 7/21/2016 3:50 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

2



NOTE: TYPE C INLET PROTECTION REQUIRED ON TWO INLETS APPROXIMATELY 100 FT. EAST OF CULVERT PIPE CROSSING ON KNOLL VIEW DRIVE

EROSION CONTROL GENERAL NOTES

1. AREAS WITHIN THE SLOPE INTERCEPTS NOT USING EROSION MAT NEED TO HAVE TOPSOIL, FERTILIZER, SEED, AND MULCH PLACED.

2. WHEN INLET PROTECTION TYPE A AND TYPE B, C, OR D ARE INDICATED AT THE SAME LOCATION, USE TYPE A PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER AND USE TYPE B, C, OR D AFTER THE INLET CASTINGS ARE IN PLACE.

3. RESTORATION OF EXPOSED SLOPES AND DITCHES SHALL TAKE PLACE NOT MORE THAN 7 DAYS AFTER FINISHED GRADING IS COMPLETE.

PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	EROSION CONTROL	SHEET 20 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_022001_EC.DWG
LAYOUT NAME - 59900036_022001_EC - EC 4
PLOT DATE : 7/21/2016 3:50 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

LEGEND

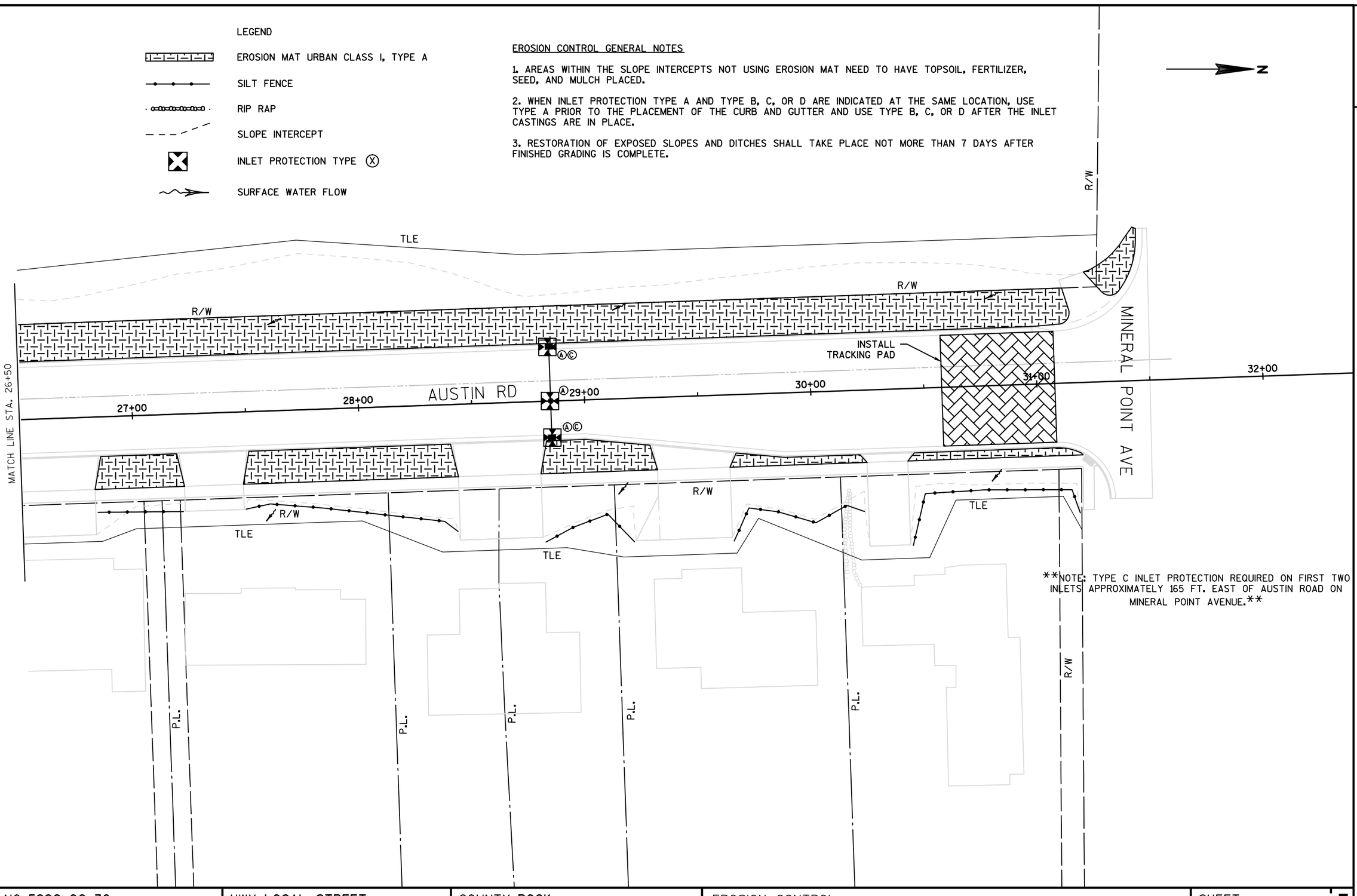
- EROSION MAT URBAN CLASS I, TYPE A
- SILT FENCE
- RIP RAP
- SLOPE INTERCEPT
- INLET PROTECTION TYPE (X)
- SURFACE WATER FLOW

EROSION CONTROL GENERAL NOTES

1. AREAS WITHIN THE SLOPE INTERCEPTS NOT USING EROSION MAT NEED TO HAVE TOPSOIL, FERTILIZER, SEED, AND MULCH PLACED.

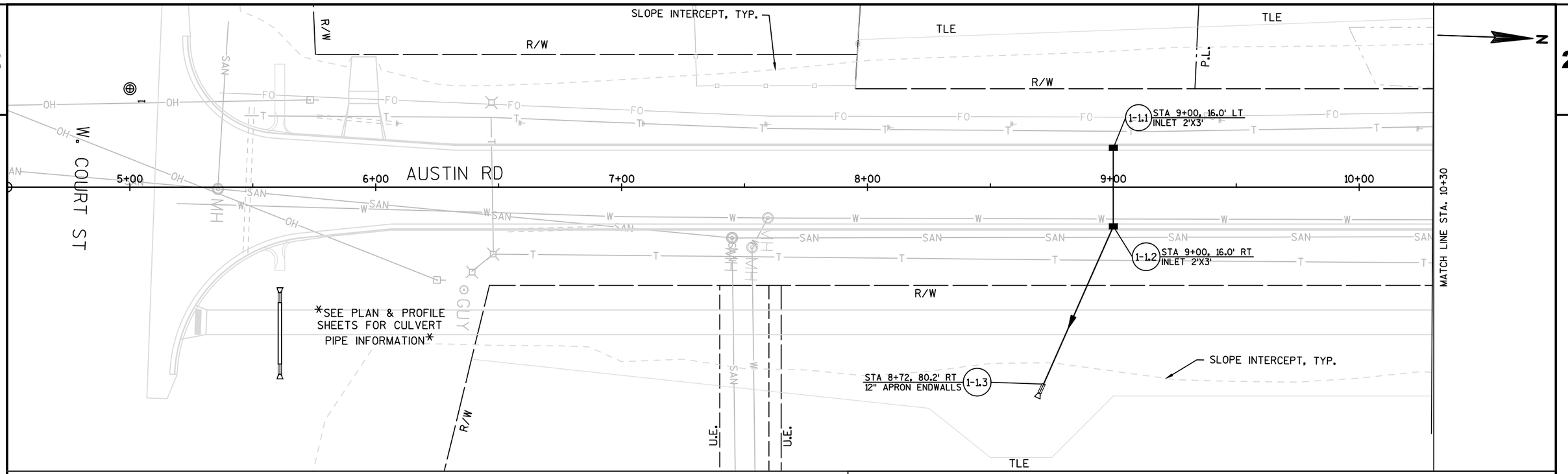
2. WHEN INLET PROTECTION TYPE A AND TYPE B, C, OR D ARE INDICATED AT THE SAME LOCATION, USE TYPE A PRIOR TO THE PLACEMENT OF THE CURB AND GUTTER AND USE TYPE B, C, OR D AFTER THE INLET CASTINGS ARE IN PLACE.

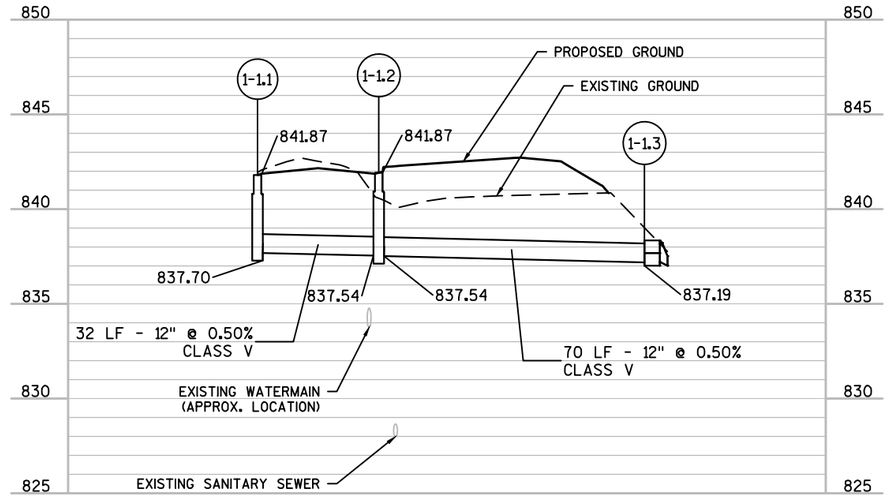
3. RESTORATION OF EXPOSED SLOPES AND DITCHES SHALL TAKE PLACE NOT MORE THAN 7 DAYS AFTER FINISHED GRADING IS COMPLETE.



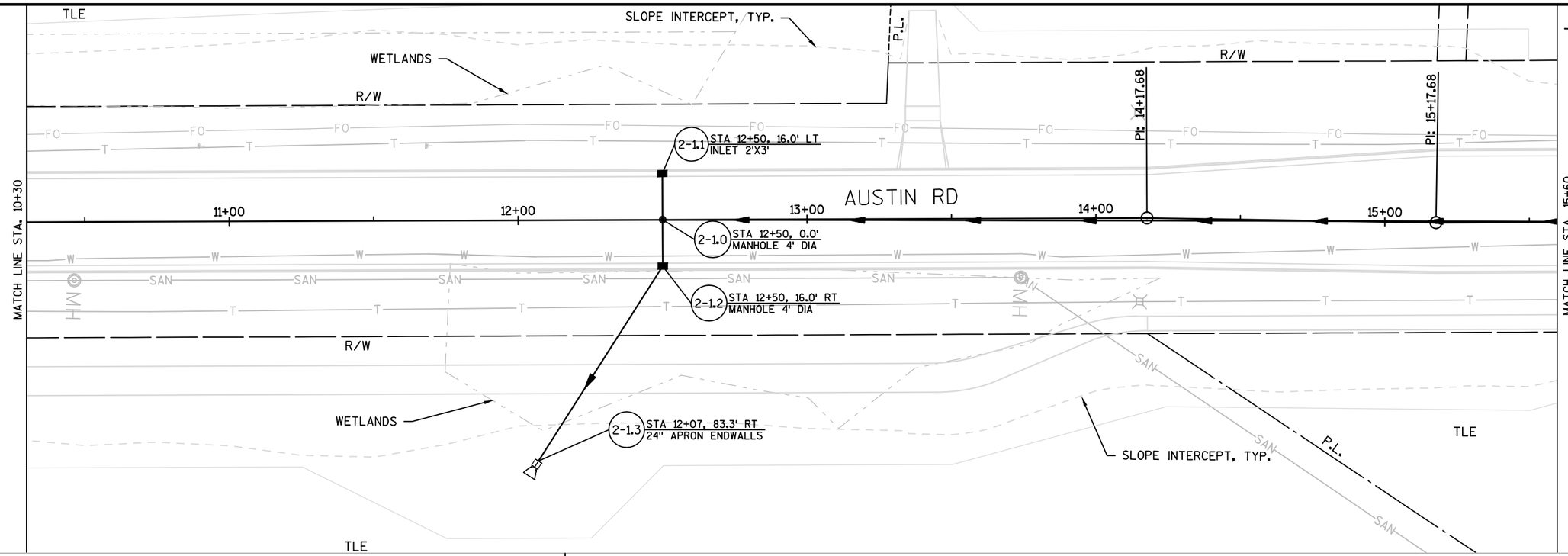
NOTE: TYPE C INLET PROTECTION REQUIRED ON FIRST TWO INLETS APPROXIMATELY 165 FT. EAST OF AUSTIN ROAD ON MINERAL POINT AVENUE.

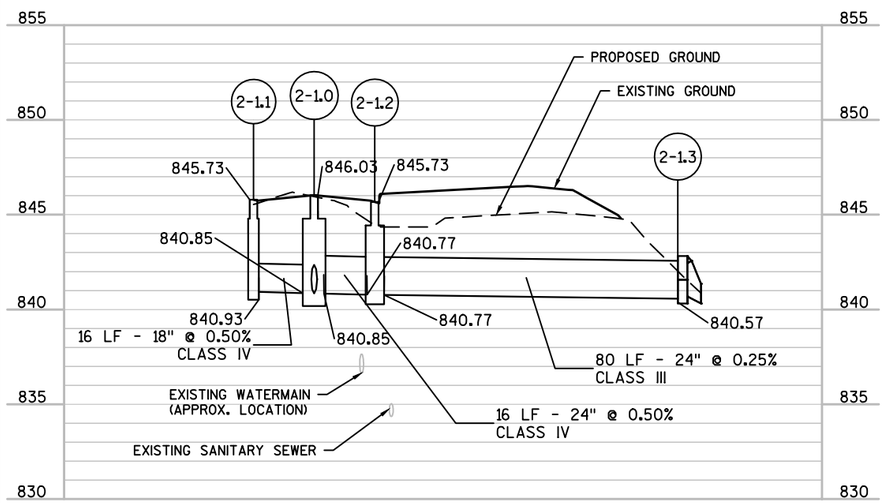
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	EROSION CONTROL	SHEET 21 E

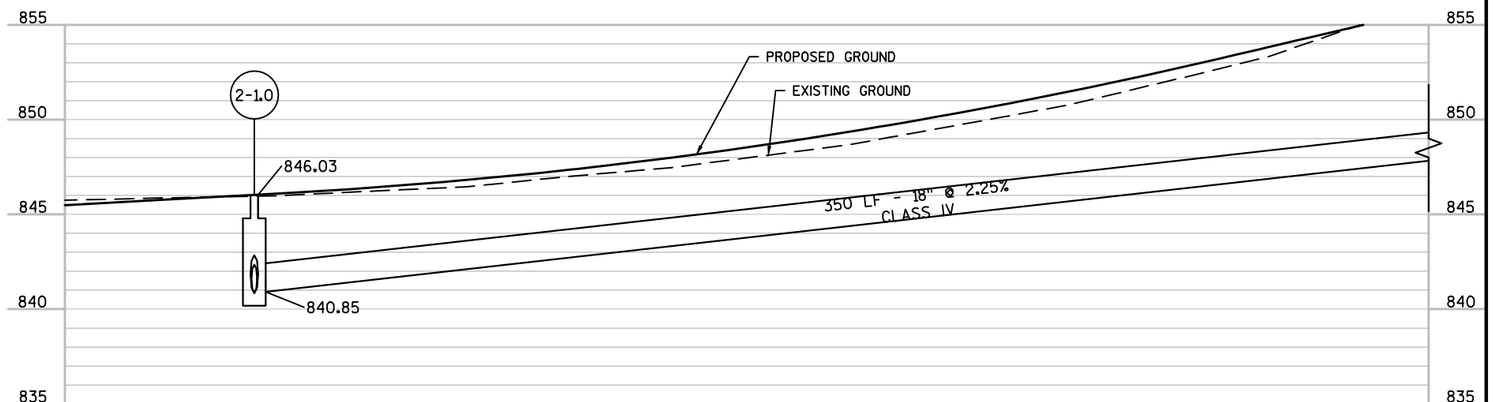




PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	STORM SEWER	SHEET 22 E

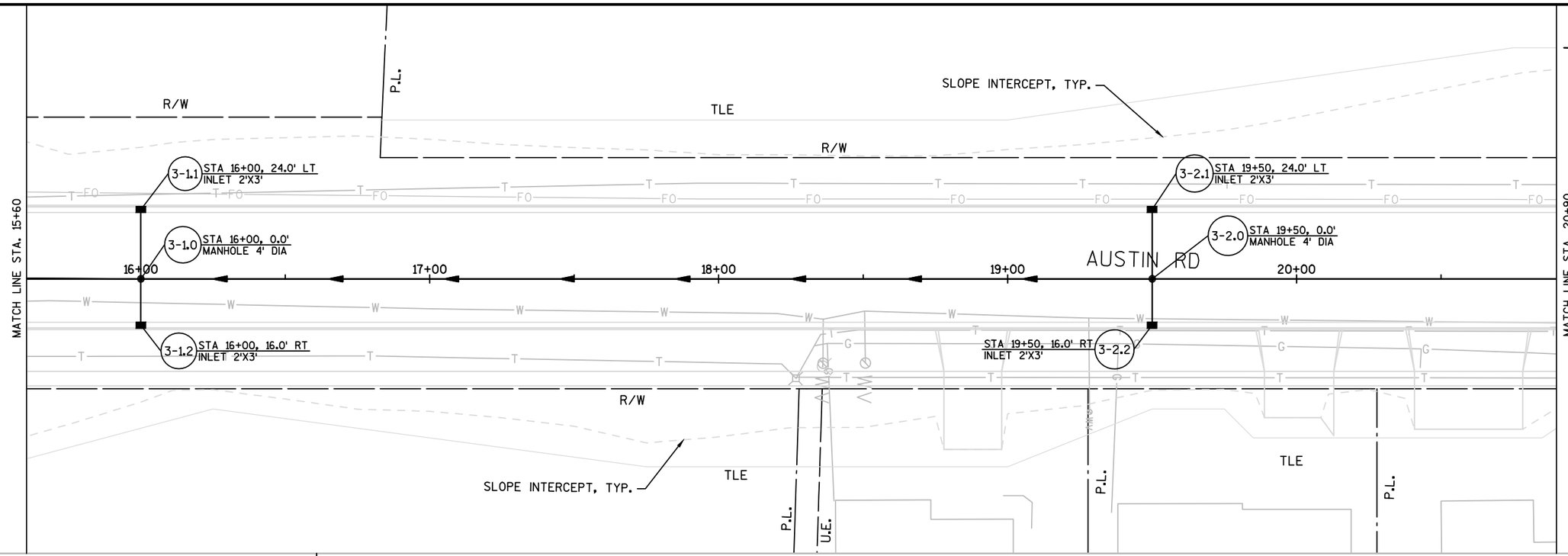


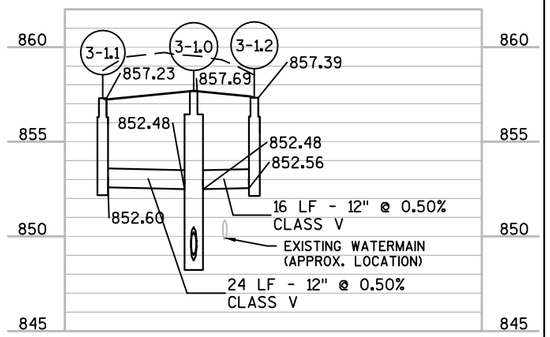


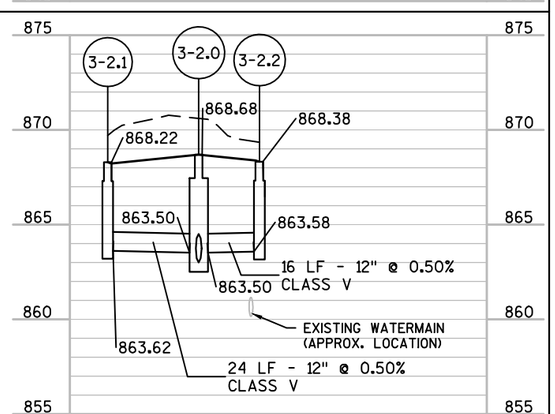


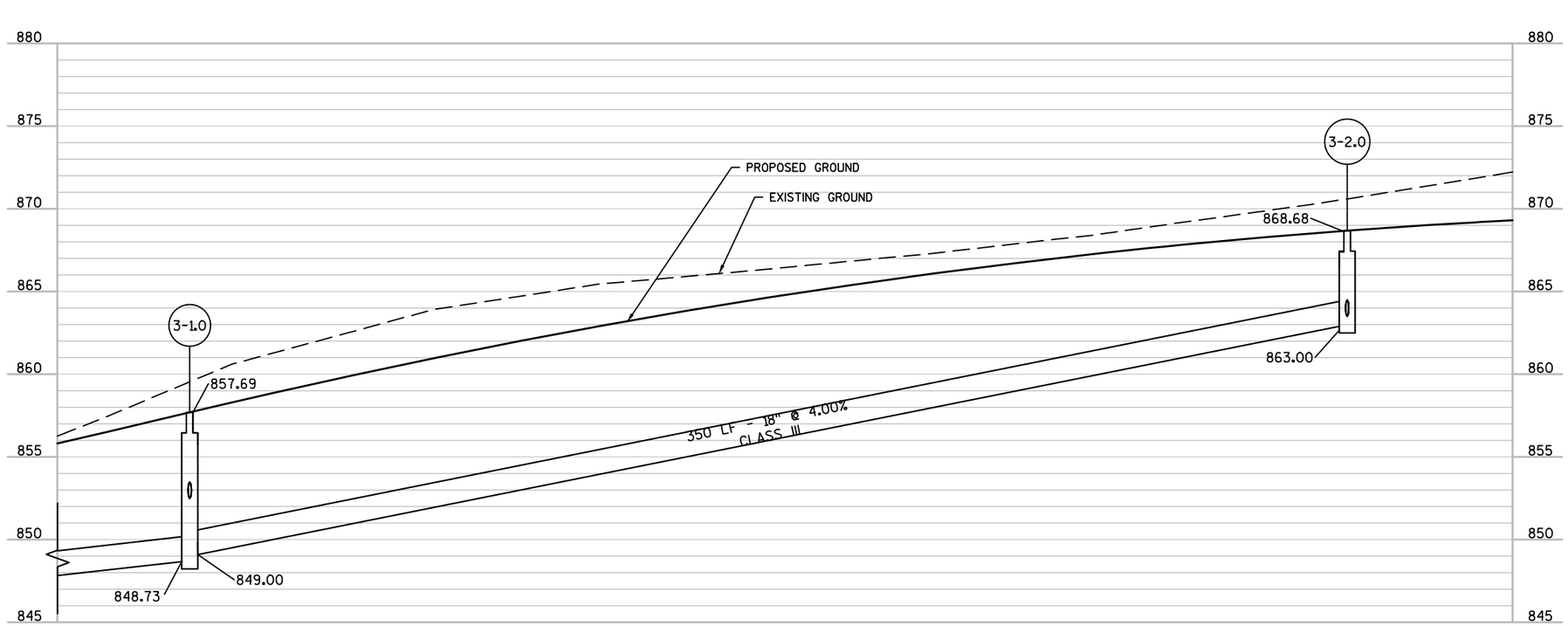
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	STORM SEWER	SHEET 23 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_022501_SS.DWG
LAYOUT NAME - 59900036_022501_SS - SS - (2)
PLOT DATE : 7/21/2016 3:56 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 41

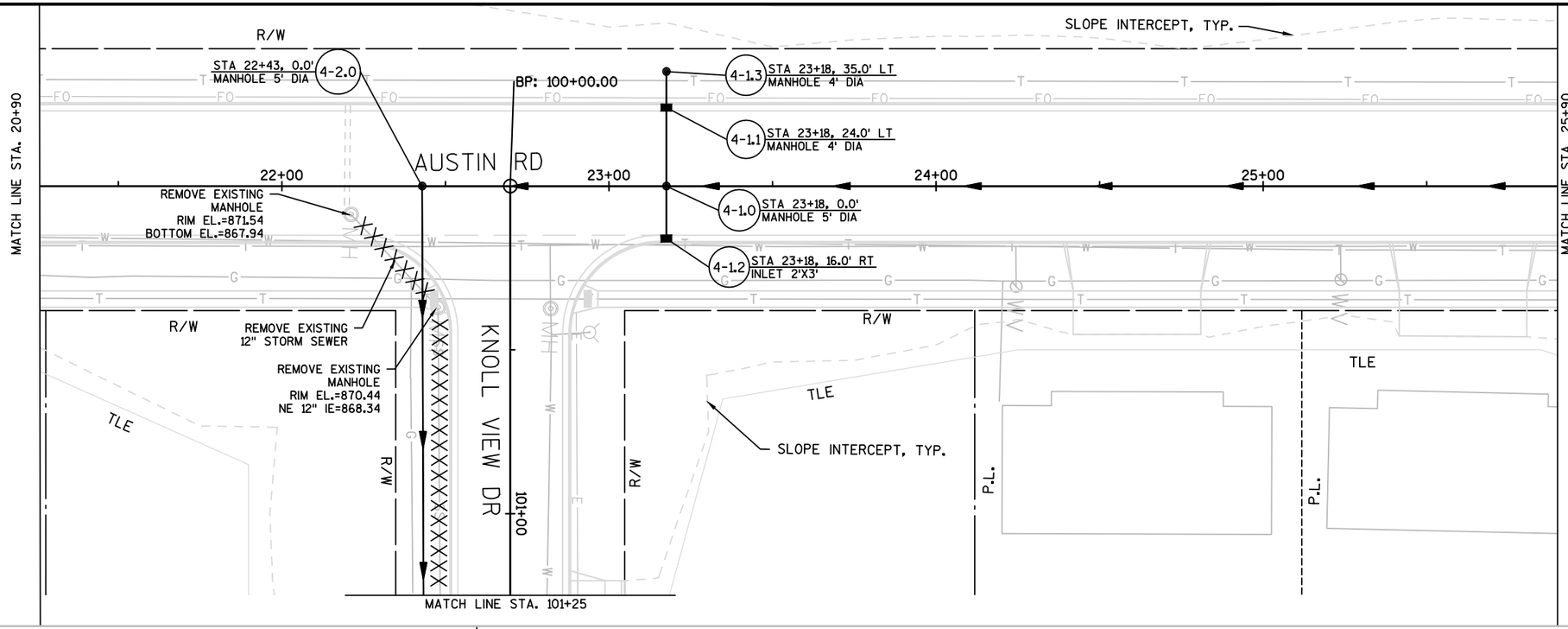


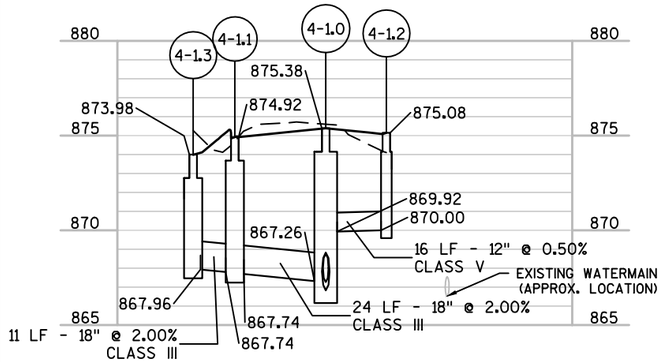


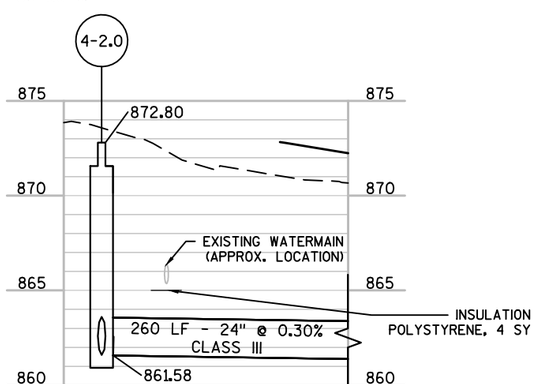


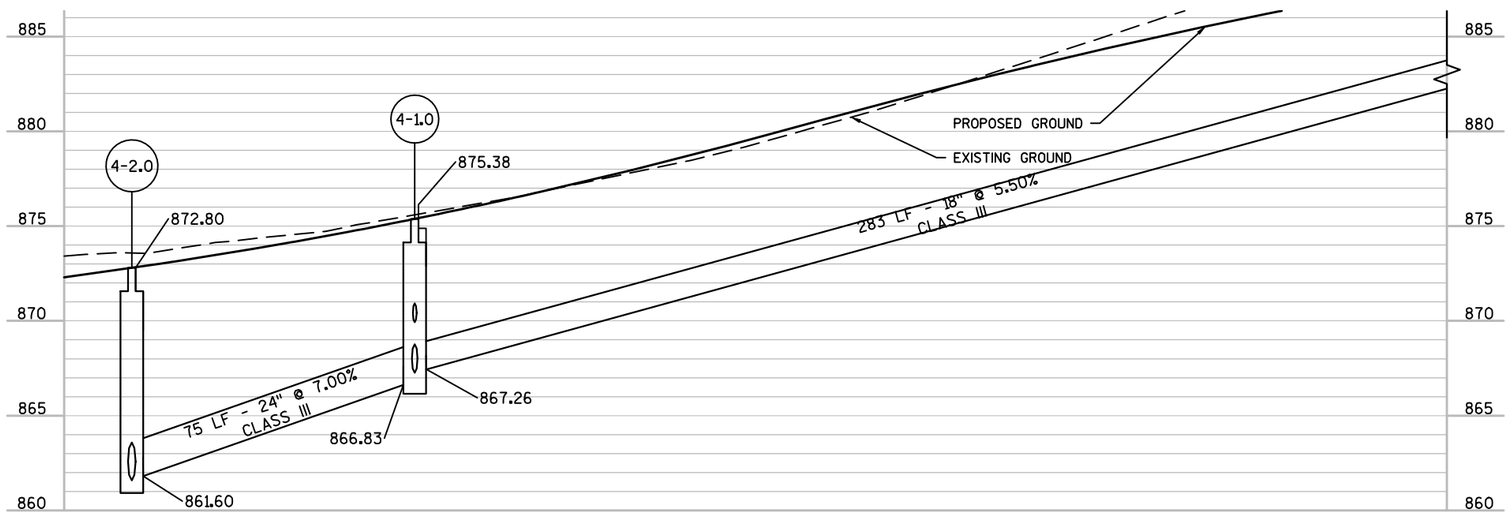


PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	STORM SEWER	SHEET 24 E

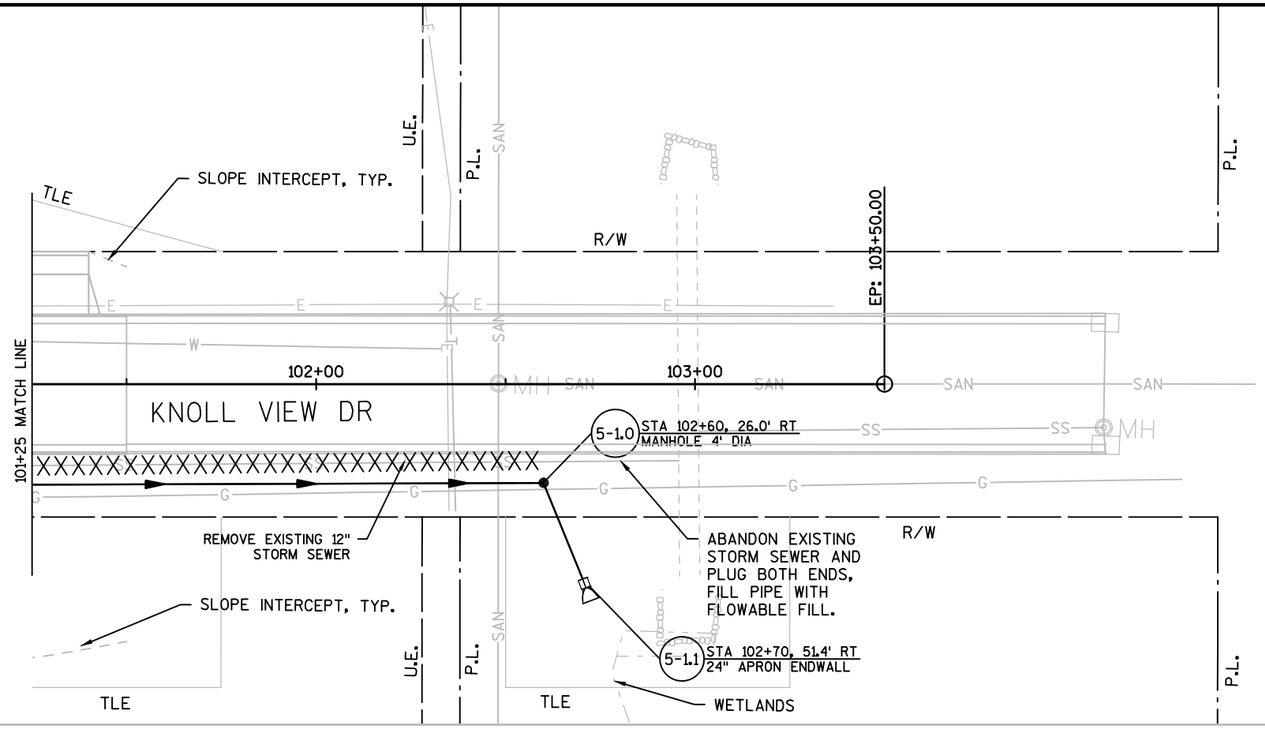


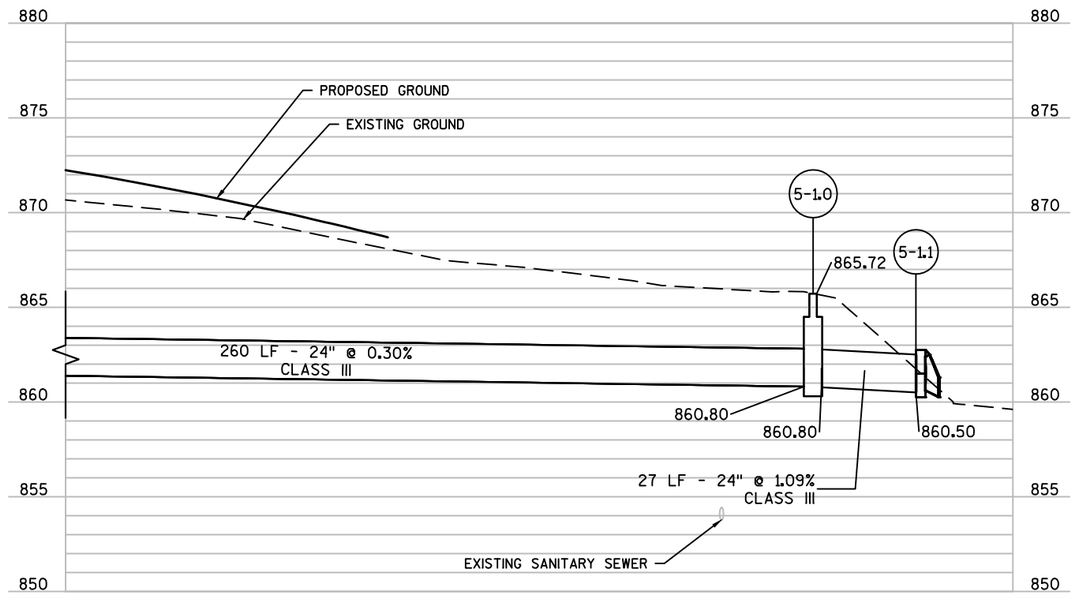




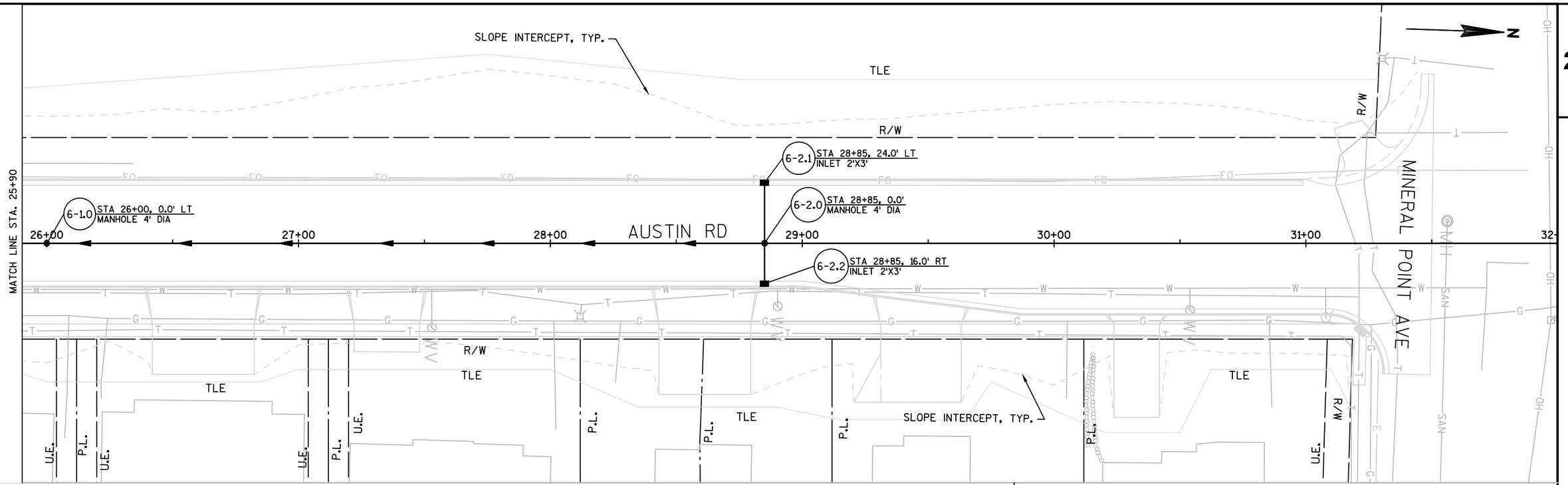


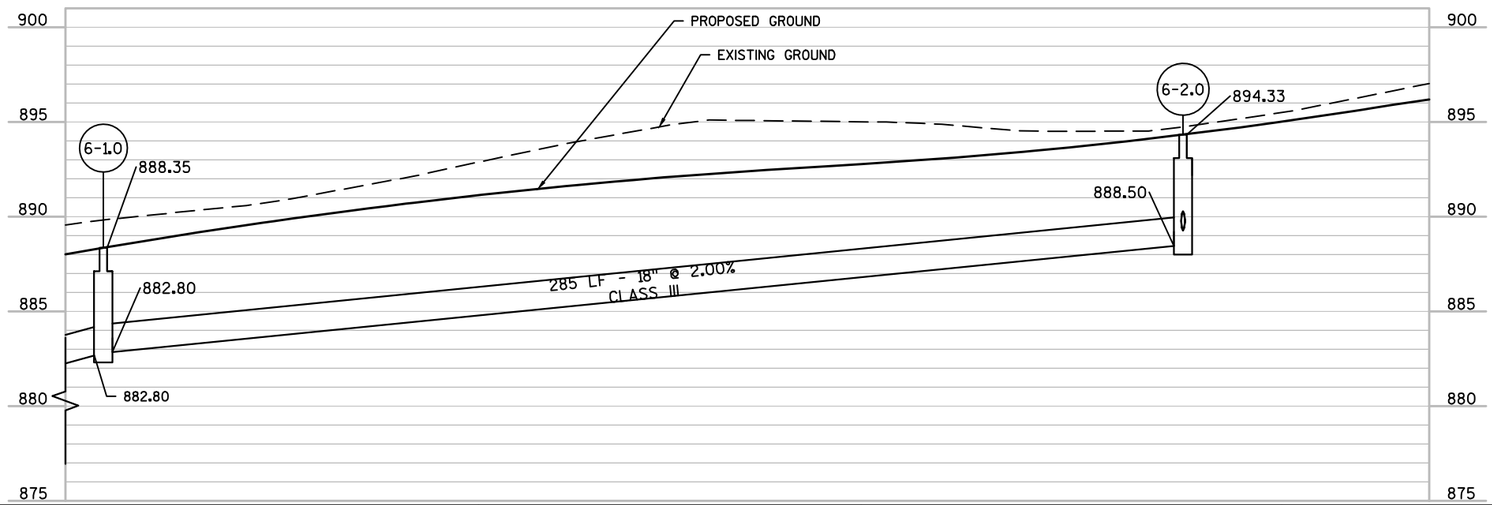
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	STORM SEWER	SHEET 25 E

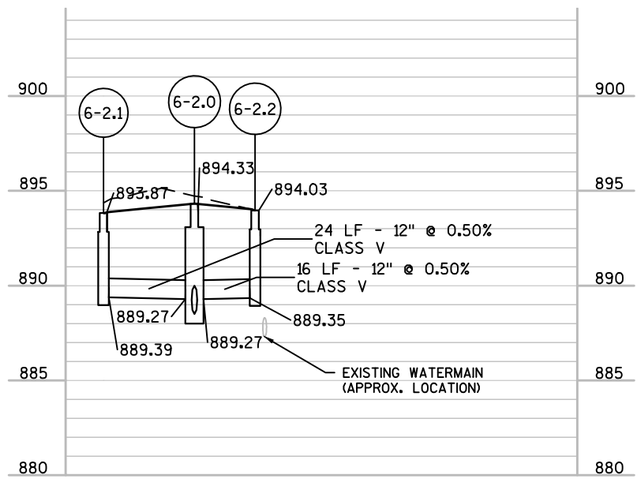




PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	STORM SEWER	SHEET 26 E



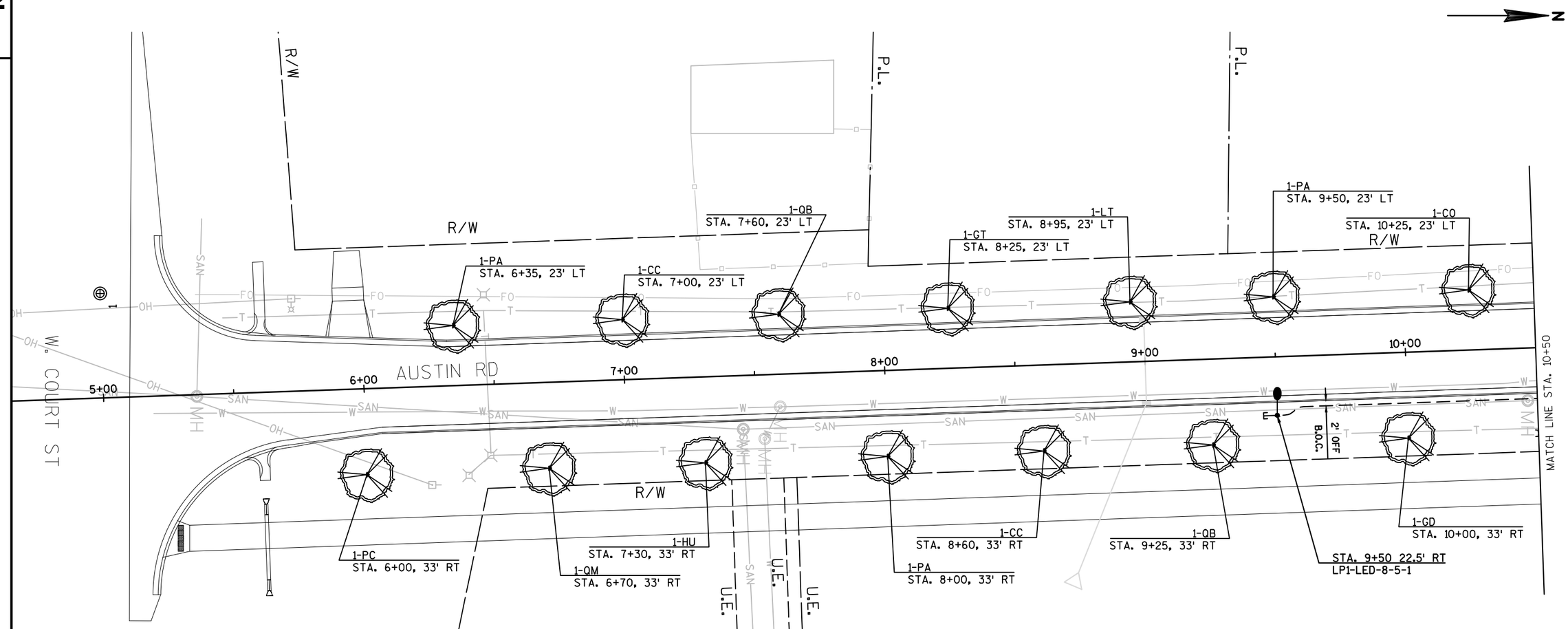




PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	STORM SEWER	SHEET 27 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_022501_SS.DWG
LAYOUT NAME - 59900036_022501_SS - SS - (6)
PLOT DATE : 7/21/2016 3:56 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 41

2



LEGEND

- PROPOSED ROADWAY LIGHT POLE, LUMINAIRE, MAST ARM
- LIGHTING PULL BOX
- ELECTRIC SERVICE
- CONDUIT 2" NON-METALLIC
- EXISTING LIGHT POLE
- EXISTING LIGHT POLE UTILITY
- TREE (SEE PLANT ITEM CHART)

ROADWAY LIGHTING UNIT NOTATION

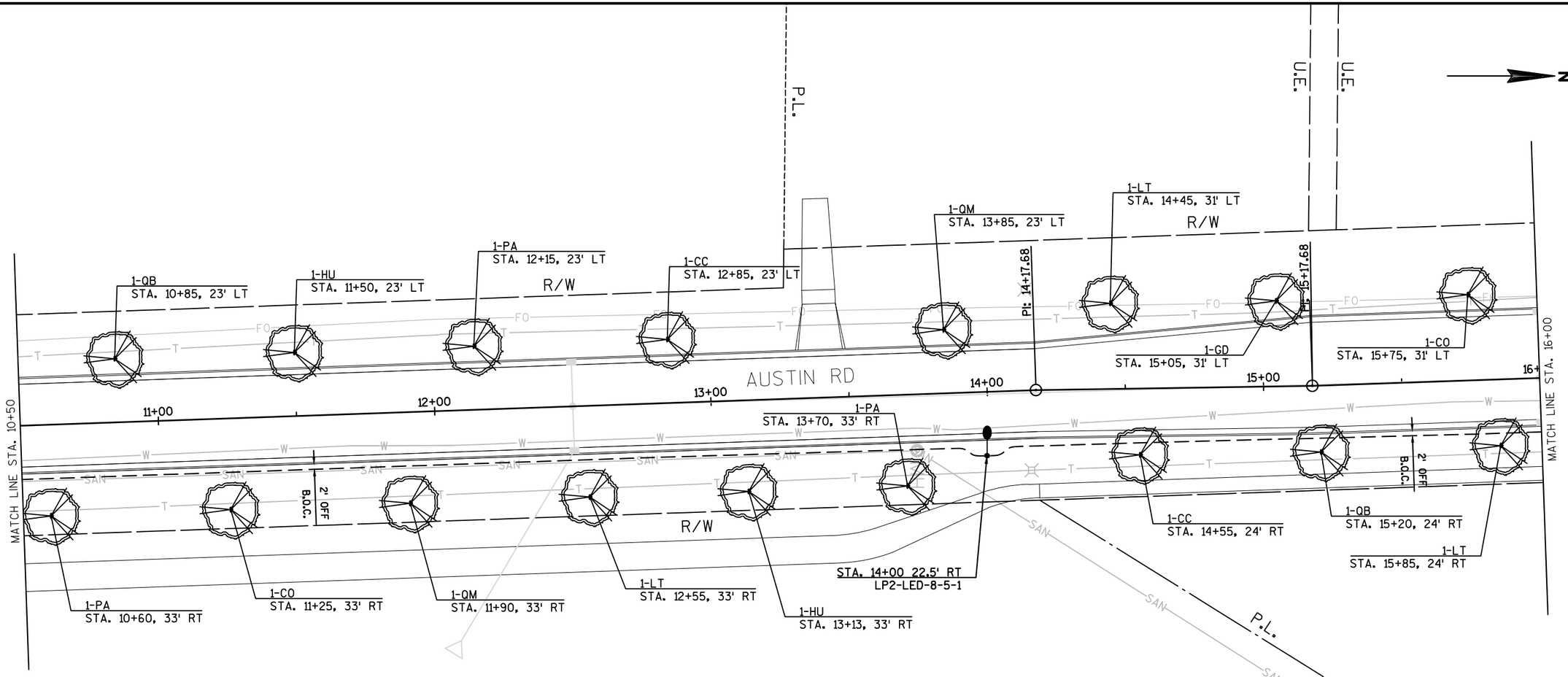
STATION, OFFSET
LP1-LED-8-5-1

- CIRCUIT NUMBER
- BASE TYPE
- ARM LENGTH IN FEET
- LAMP TYPE
- LIGHT DESIGNATION

PLANT ITEM

QUANTITY	SYMBOL	COMMON NAME	SCIENTIFIC NAME	*SIZE WHEN PLANTED	ROOT ZONE METHOD	MINIMUM SIZE				MULCH RING DIAMETER
						BALL/POT SIZE	BALL/POT DEPTH	PLANT HOLE DIAMETER	PLANT HOLE DEPTH	
2	CC	Turkish Filbert	Corylus colurna	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	CO	Hackberry	Celtis occidentalis	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	GD	Kentucky Coffee Tree	Gymnocladus dioica 'Espresso'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	GT	Honey Locust	Gleditsia triacanthos	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	HU	Hybrid Elm	Ulmus americana	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	LT	Tulip Tree	Liriodendron tulipifera	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
3	PA	Plane Tree	Platanus acerfolia	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	PC	Autumn Blaze Pear	Pyrus calleryana 'Autumn Blaze'	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	QB	Swamp White Oak	Quercus bicolor	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	QM	Bur Oak	Quercus macrocarpa	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
-	QR	Regal Prince Oak	Quercus robur fastigiata x bicolor 'Long'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"

2



LEGEND

- PROPOSED ROADWAY LIGHT POLE, LUMINAIRE, MAST ARM
- LIGHTING PULL BOX
- ELECTRIC SERVICE
- CONDUIT 2" NON-METALLIC
- EXISTING LIGHT POLE
- EXISTING LIGHT POLE UTILITY
- TREE (SEE PLANT ITEM CHART)

ROADWAY LIGHTING UNIT NOTATION

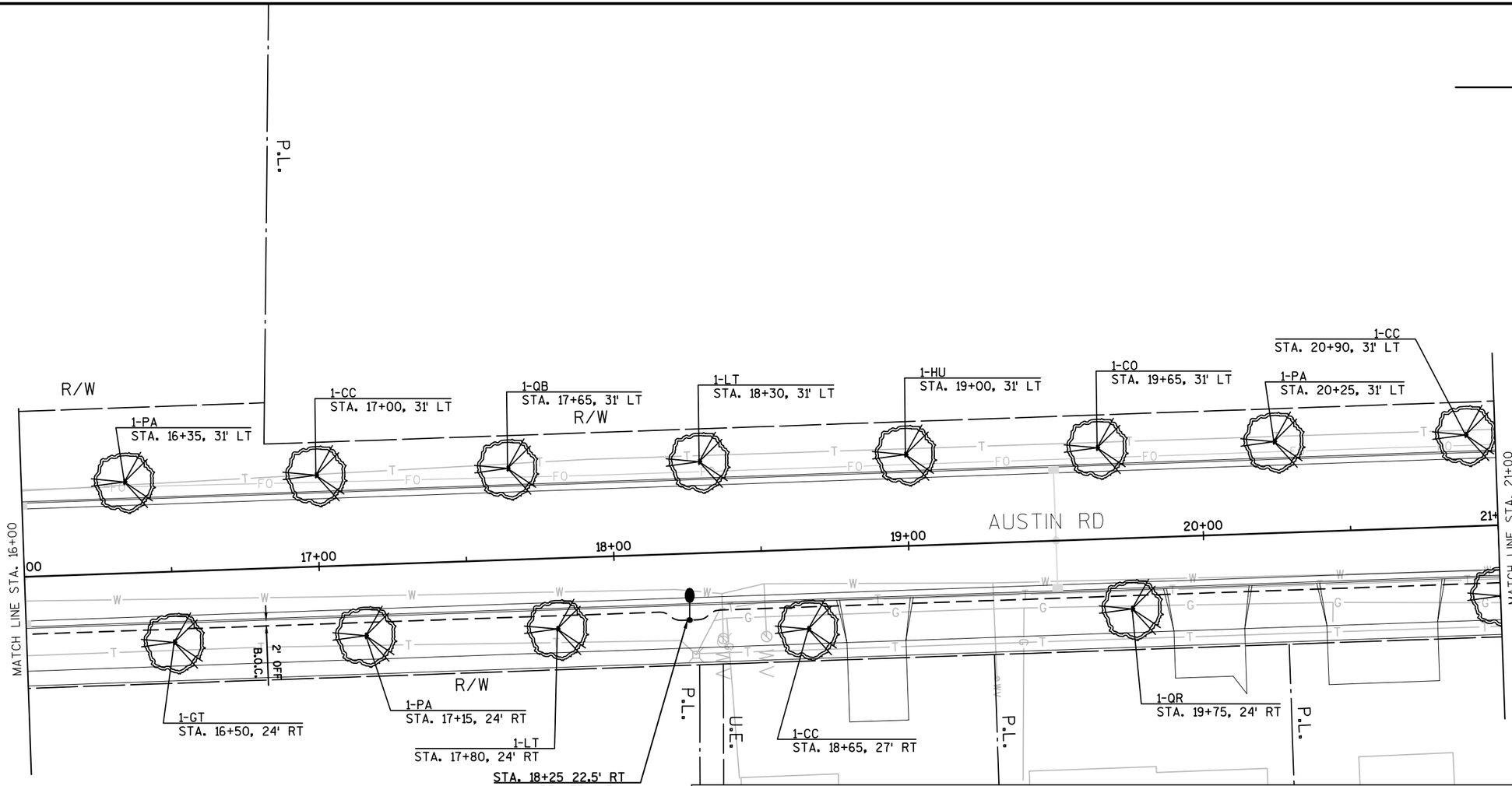
STATION, OFFSET
LP1-LED-8-5-1

- CIRCUIT NUMBER
- BASE TYPE
- ARM LENGTH IN FEET
- LAMP TYPE
- LIGHT DESIGNATION

PLANT ITEM

QUANTITY	SYMBOL	COMMON NAME	SCIENTIFIC NAME	*SIZE WHEN PLANTED	ROOT ZONE METHOD	MINIMUM SIZE				MULCH RING DIAMETER
						BALL/POT SIZE	BALL/POT DEPTH	PLANT HOLE DIAMETER	PLANT HOLE DEPTH	
2	CC	Turkish Filbert	Corylus colurna	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
2	CO	Hackberry	Celtis occidentalis	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	GD	Kentucky Coffee Tree	Gymnocladus dioica 'Espresso'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
-	GT	Honey Locust	Gleditsia triacanthos	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	HU	Hybrid Elm	Ulmus americana	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
3	LT	Tulip Tree	Liriodendron tulipifera	3" Cal.	B&B	24"	16"	48"		5'-0"
3	PA	Plane Tree	Platanus acerfolia	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
-	PC	Autumn Blaze Pear	Pyrus calleryana 'Autumn Blaze'	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	QB	Swamp White Oak	Quercus bicolor	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	QM	Bur Oak	Quercus macrocarpa	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
-	QR	Regal Prince Oak	Quercus robur fastigiata x bicolor 'Long'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"

2



LEGEND

- PROPOSED ROADWAY LIGHT POLE, LUMINAIRE, MAST ARM
- LIGHTING PULL BOX
- ELECTRIC SERVICE
- CONDUIT 2" NON-METALLIC
- EXISTING LIGHT POLE
- EXISTING LIGHT POLE UTILITY
- TREE (SEE PLANT ITEM CHART)

ROADWAY LIGHTING UNIT NOTATION

STATION, OFFSET
LP1-LED-8-5-1

- CIRCUIT NUMBER
- BASE TYPE
- ARM LENGTH IN FEET
- LAMP TYPE
- LIGHT DESIGNATION

PLANT ITEM

QUANTITY	SYMBOL	COMMON NAME	SCIENTIFIC NAME	*SIZE WHEN PLANTED	ROOT ZONE METHOD	MINIMUM SIZE				MULCH RING DIAMETER
						BALL/POT SIZE	BALL/POT DEPTH	PLANT HOLE DIAMETER	PLANT HOLE DEPTH	
3	CC	Turkish Filbert	Corylus colurna	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	CO	Hackberry	Celtis occidentalis	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
-	GD	Kentucky Coffee Tree	Gymnocladus dioica 'Espresso'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	GT	Honey Locust	Gleditsia triacanthos	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	HU	Hybrid Elm	Ulmus americana	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	LT	Tulip Tree	Liriodendron tulipifera	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
3	PA	Plane Tree	Platanus acerfolia	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
-	PC	Autumn Blaze Pear	Pyrus calleryana 'Autumn Blaze'	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	QB	Swamp White Oak	Quercus bicolor	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
-	QM	Bur Oak	Quercus macrocarpa	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	QR	Regal Prince Oak	Quercus robur fastigiata x bicolor 'Long'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	LIGHTING & LANDSCAPING	SHEET 30 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_023601_LP.DWG
LAYOUT NAME - 59900036_023601_LP - LIGHTING 3
PLOT DATE : 7/21/2016 4:00 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

LEGEND

- PROPOSED ROADWAY LIGHT POLE, LUMINAIRE, MAST ARM
- LIGHTING PULL BOX
- ELECTRIC SERVICE
- CONDUIT 2" NON-METALLIC
- EXISTING LIGHT POLE
- EXISTING LIGHT POLE UTILITY
- TREE (SEE PLANT ITEM CHART)

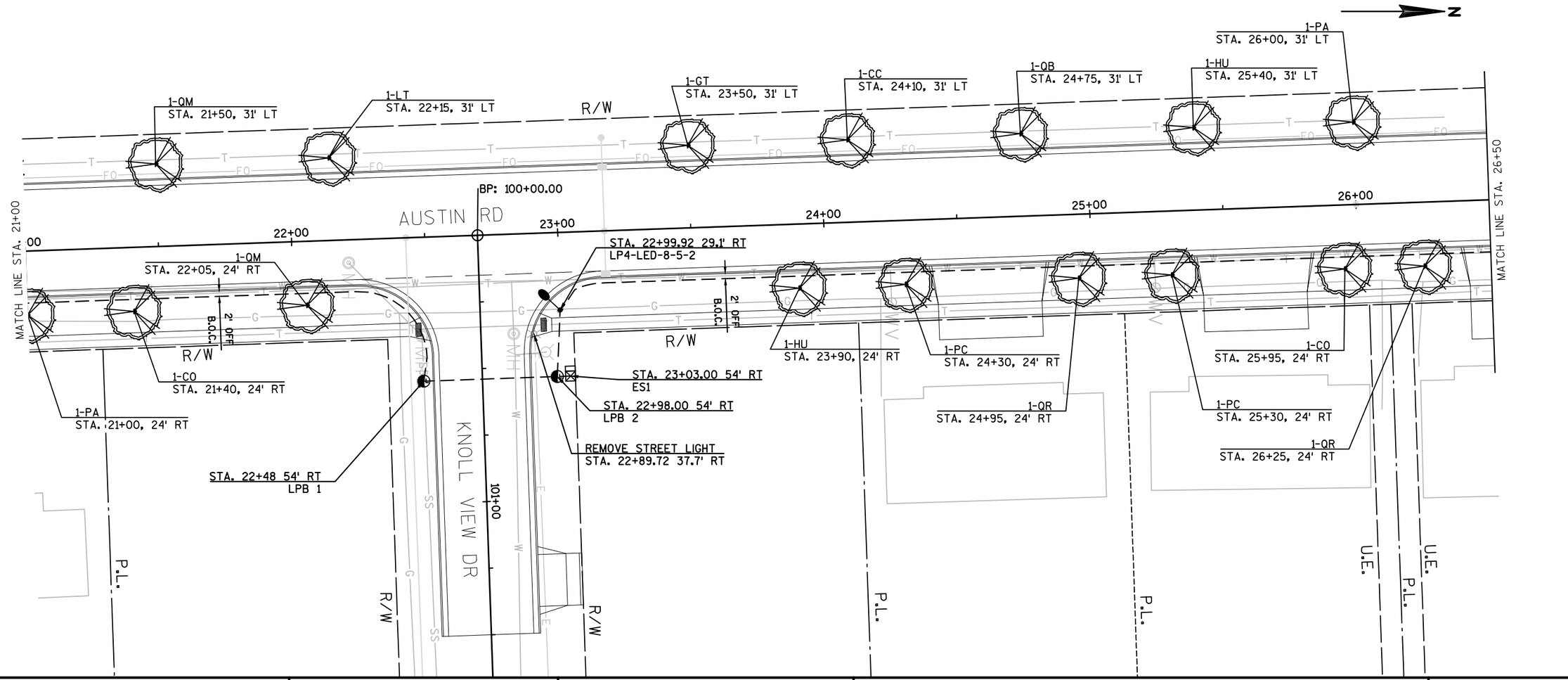
ROADWAY LIGHTING UNIT NOTATION

STATION, OFFSET
LP1-LED-8-5-1

- CIRCUIT NUMBER
- BASE TYPE
- ARM LENGTH IN FEET
- LAMP TYPE
- LIGHT DESIGNATION

PLANT ITEM

QUANTITY	SYMBOL	COMMON NAME	SCIENTIFIC NAME	*SIZE WHEN PLANTED	ROOT ZONE METHOD	MINIMUM SIZE				MULCH RING DIAMETER
						BALL/POT SIZE	BALL/POT DEPTH	PLANT HOLE DIAMETER	PLANT HOLE DEPTH	
1	CC	Turkish Filbert	Corylus colurna	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
2	CO	Hackberry	Celtis occidentalis	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
-	GD	Kentucky Coffee Tree	Gymnocladus dioica 'Espresso'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	GT	Honey Locust	Gleditsia triacanthos	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	HU	Hybrid Elm	Ulmus americana	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	LT	Tulip Tree	Liriodendron tulipifera	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	PA	Plane Tree	Platanus acerfolia	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
2	PC	Autumn Blaze Pear	Pyrus calleryana 'Autumn Blaze'	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	QB	Swamp White Oak	Quercus bicolor	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
2	QM	Bur Oak	Quercus macrocarpa	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
2	QR	Regal Prince Oak	Quercus robur fastigiata x bicolor 'Long'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	LIGHTING & LANDSCAPING	SHEET 31 E

2

LEGEND

- PROPOSED ROADWAY LIGHT POLE, LUMINAIRE, MAST ARM
- LIGHTING PULL BOX
- ELECTRIC SERVICE
- CONDUIT 2" NON-METALLIC
- EXISTING LIGHT POLE
- EXISTING LIGHT POLE UTILITY
- TREE (SEE PLANT ITEM CHART)

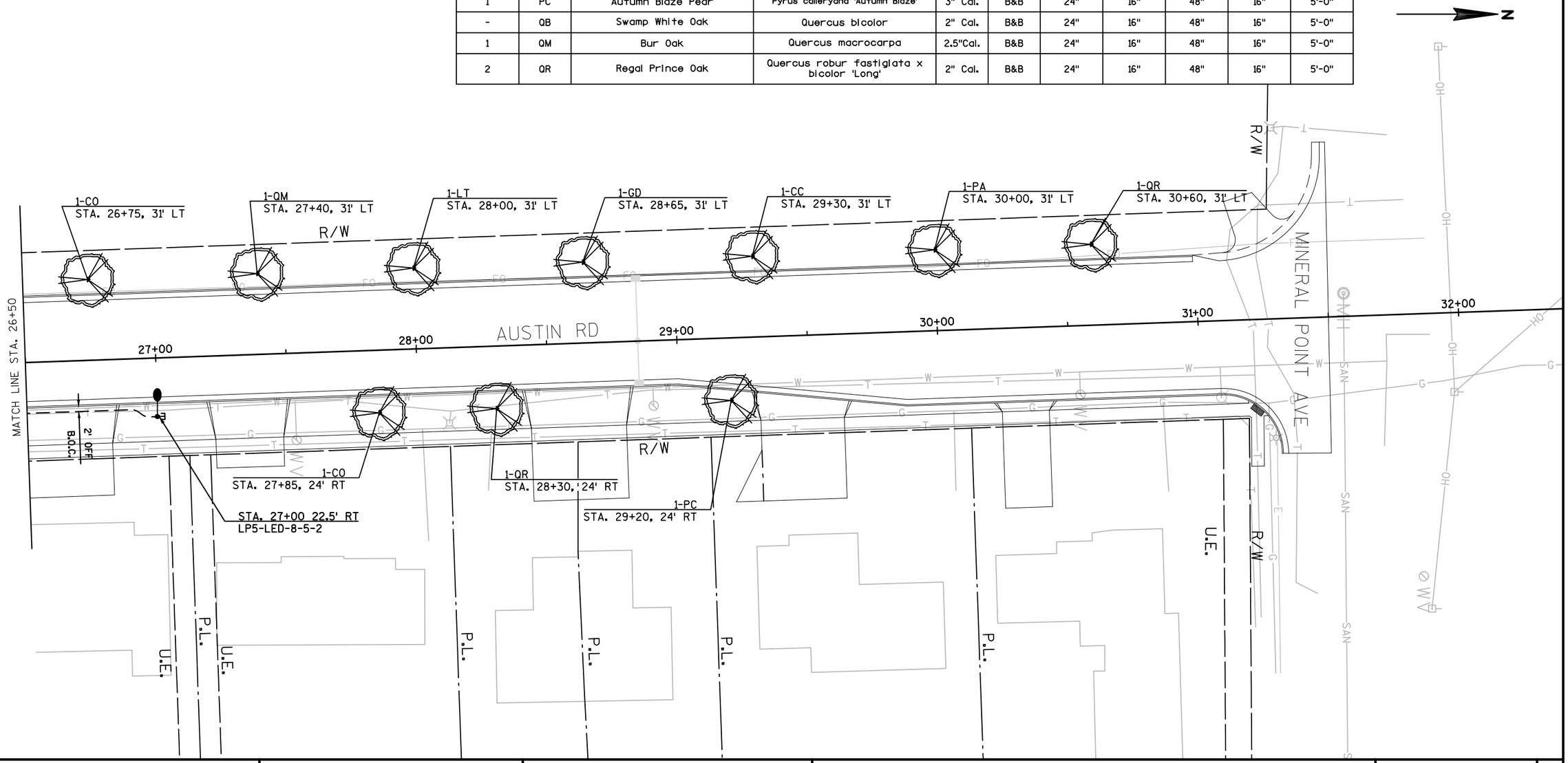
ROADWAY LIGHTING UNIT NOTATION

STATION, OFFSET
LP1-LED-8-5-1

- CIRCUIT NUMBER
- BASE TYPE
- ARM LENGTH IN FEET
- LAMP TYPE
- LIGHT DESIGNATION

PLANT ITEM

QUANTITY	SYMBOL	COMMON NAME	SCIENTIFIC NAME	*SIZE WHEN PLANTED	ROOT ZONE METHOD	MINIMUM SIZE				MULCH RING DIAMETER
						BALL/POT SIZE	BALL/POT DEPTH	PLANT HOLE DIAMETER	PLANT HOLE DEPTH	
1	CC	Turkish Filbert	Corylus colurna	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
2	CO	Hackberry	Celtis occidentalis	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	GD	Kentucky Coffee Tree	Gymnocladus dioica 'Espresso'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
-	GT	Honey Locust	Gleditsia triacanthos	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
-	HU	Hybrid Elm	Ulmus americana	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	LT	Tulip Tree	Liriodendron tulipifera	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	PA	Plane Tree	Platanus acerfolia	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
1	PC	Autumn Blaze Pear	Pyrus calleryana 'Autumn Blaze'	3" Cal.	B&B	24"	16"	48"	16"	5'-0"
-	QB	Swamp White Oak	Quercus bicolor	2" Cal.	B&B	24"	16"	48"	16"	5'-0"
1	QM	Bur Oak	Quercus macrocarpa	2.5"Cal.	B&B	24"	16"	48"	16"	5'-0"
2	QR	Regal Prince Oak	Quercus robur fastigiata x bicolor 'Long'	2" Cal.	B&B	24"	16"	48"	16"	5'-0"

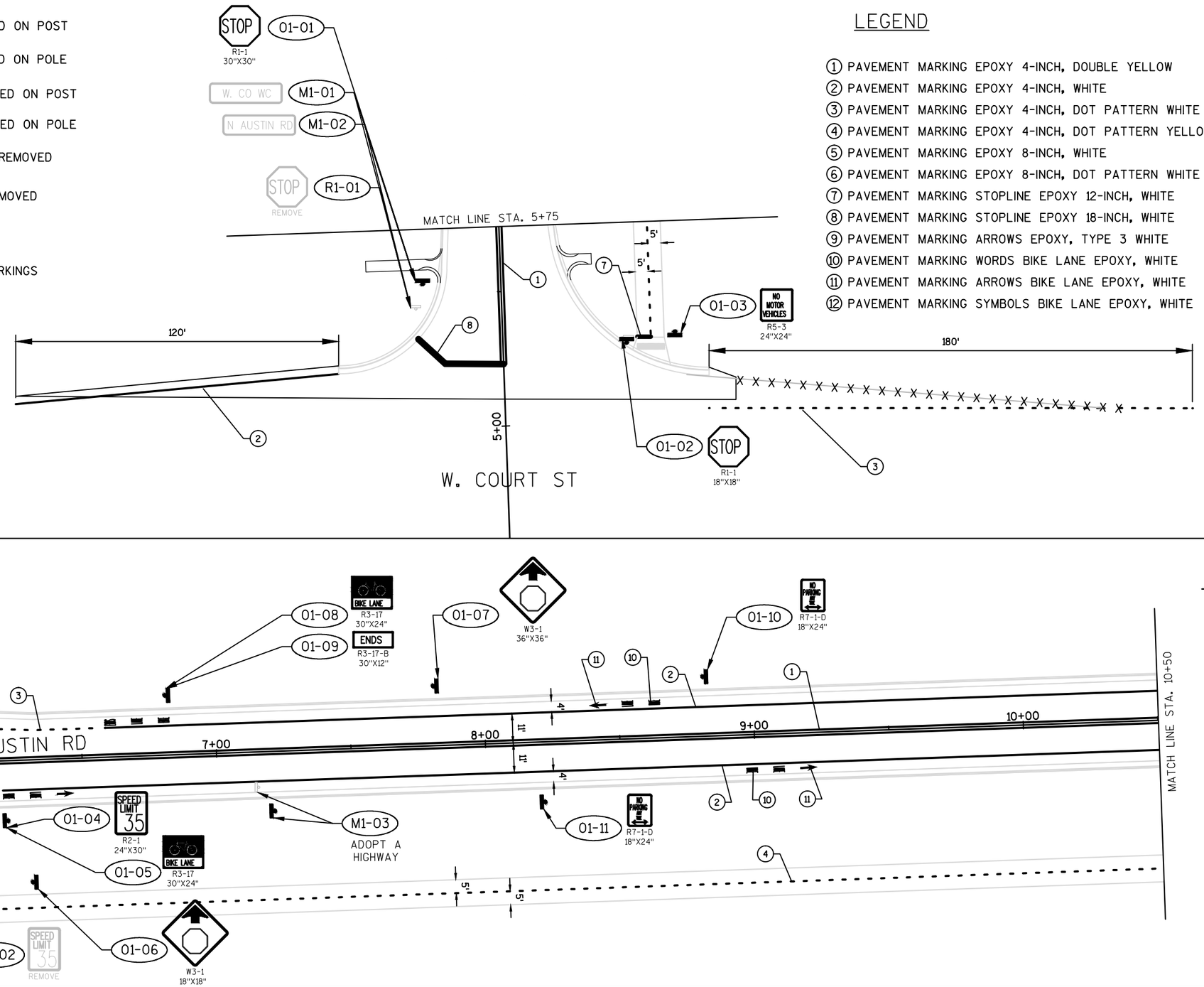


PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	LIGHTING & LANDSCAPING	SHEET 32 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_023601_LP.DWG
LAYOUT NAME - 59900036_023601_LP - LIGHTING 5
PLOT DATE : 7/21/2016 4:00 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

LEGEND

- EXISTING SIGN MOUNTED ON POST
- EXISTING SIGN MOUNTED ON POLE
- PROPOSED SIGN MOUNTED ON POST
- PROPOSED SIGN MOUNTED ON POLE
- RX-XX — DENOTES SIGN TO BE REMOVED
- MX-XX — DENOTES SIGN TO BE MOVED
- XX-XX — DENOTES SIGN NUMBER
- X X X X X — REMOVE PAVEMENT MARKINGS



LEGEND

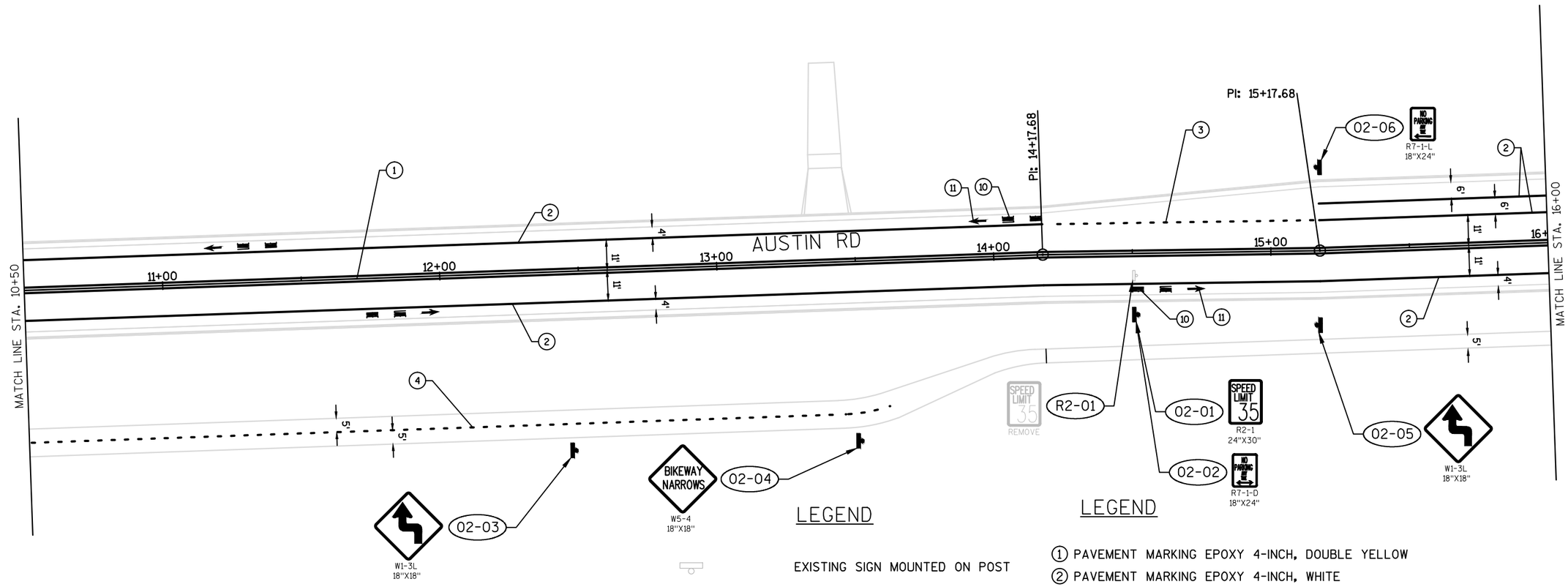
1. PAVEMENT MARKING EPOXY 4-INCH, DOUBLE YELLOW
2. PAVEMENT MARKING EPOXY 4-INCH, WHITE
3. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
4. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN YELLOW (3 FT LINE 9 FT SKIP)
5. PAVEMENT MARKING EPOXY 8-INCH, WHITE
6. PAVEMENT MARKING EPOXY 8-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
7. PAVEMENT MARKING STOPLINE EPOXY 12-INCH, WHITE
8. PAVEMENT MARKING STOPLINE EPOXY 18-INCH, WHITE
9. PAVEMENT MARKING ARROWS EPOXY, TYPE 3 WHITE
10. PAVEMENT MARKING WORDS BIKE LANE EPOXY, WHITE
11. PAVEMENT MARKING ARROWS BIKE LANE EPOXY, WHITE
12. PAVEMENT MARKING SYMBOLS BIKE LANE EPOXY, WHITE

N

Z

PROJECT NO:5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PAVEMENT MARKING & SIGNING	SHEET 33 E

2



LEGEND

EXISTING SIGN MOUNTED ON POST

EXISTING SIGN MOUNTED ON POLE

PROPOSED SIGN MOUNTED ON POST

PROPOSED SIGN MOUNTED ON POLE

RX-XX — DENOTES SIGN TO BE REMOVED

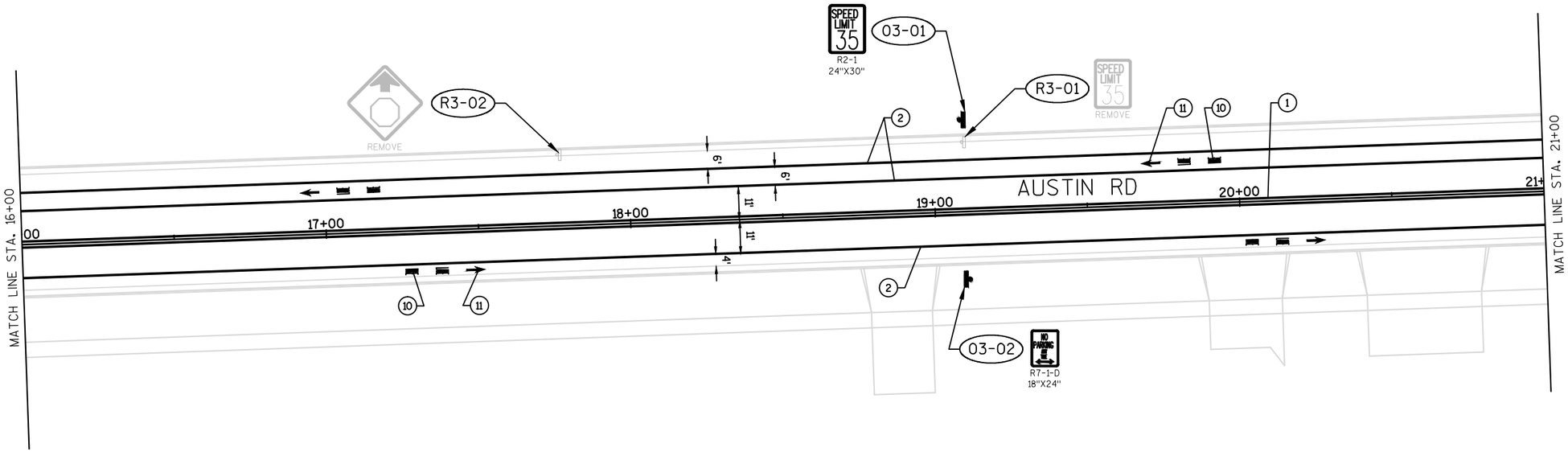
MX-XX — DENOTES SIGN TO BE MOVED

XX-XX — DENOTES SIGN NUMBER

LEGEND

① PAVEMENT MARKING EPOXY 4-INCH, DOUBLE YELLOW
② PAVEMENT MARKING EPOXY 4-INCH, WHITE
③ PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
④ PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN YELLOW (3 FT LINE 9 FT SKIP)
⑤ PAVEMENT MARKING EPOXY 8-INCH, WHITE
⑥ PAVEMENT MARKING EPOXY 8-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
⑦ PAVEMENT MARKING STOPLINE EPOXY 12-INCH, WHITE
⑧ PAVEMENT MARKING STOPLINE EPOXY 18-INCH, WHITE
⑨ PAVEMENT MARKING ARROWS EPOXY, TYPE 3 WHITE
⑩ PAVEMENT MARKING WORDS BIKE LANE EPOXY, WHITE
⑪ PAVEMENT MARKING ARROWS BIKE LANE EPOXY, WHITE
⑫ PAVEMENT MARKING SYMBOLS BIKE LANE EPOXY, WHITE

2



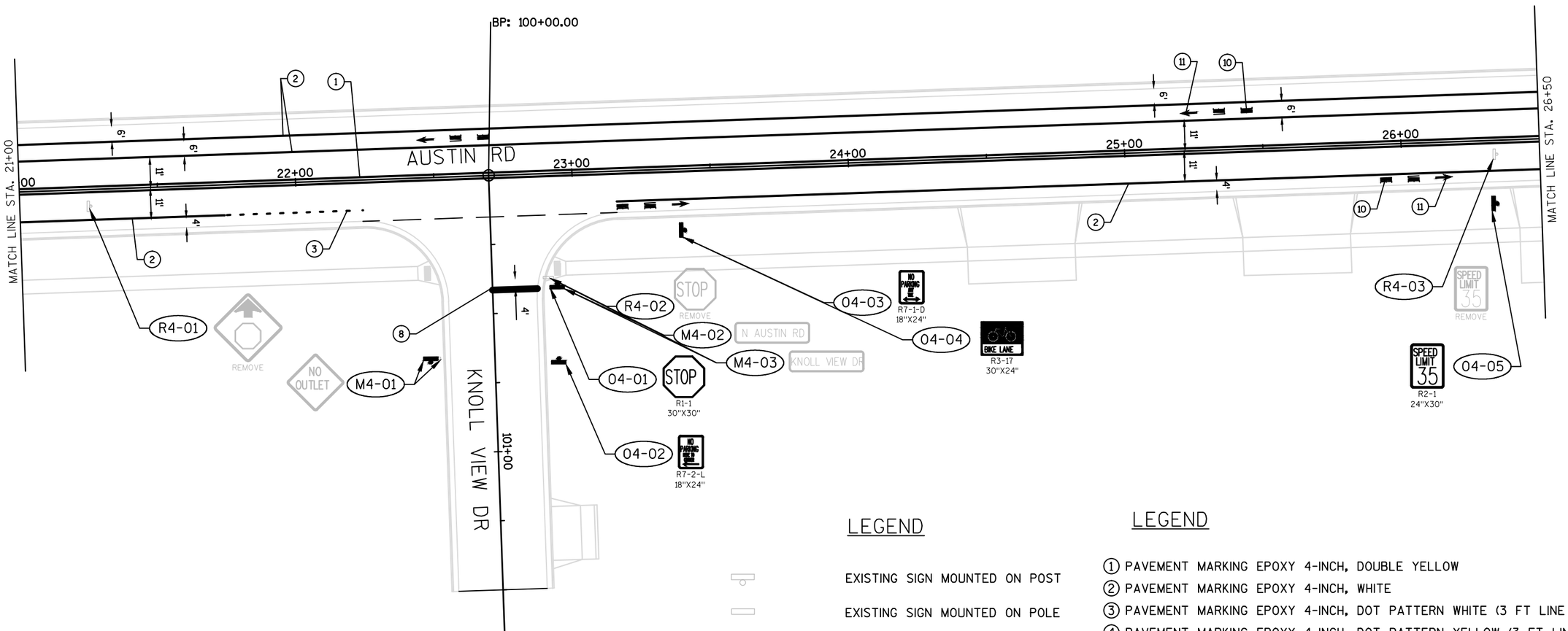
LEGEND

	EXISTING SIGN MOUNTED ON POST
	EXISTING SIGN MOUNTED ON POLE
	PROPOSED SIGN MOUNTED ON POST
	PROPOSED SIGN MOUNTED ON POLE
RX-XX	DENOTES SIGN TO BE REMOVED
MX-XX	DENOTES SIGN TO BE MOVED
XX-XX	DENOTES SIGN NUMBER

LEGEND

1. PAVEMENT MARKING EPOXY 4-INCH, DOUBLE YELLOW
2. PAVEMENT MARKING EPOXY 4-INCH, WHITE
3. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
4. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN YELLOW (3 FT LINE 9 FT SKIP)
5. PAVEMENT MARKING EPOXY 8-INCH, WHITE
6. PAVEMENT MARKING EPOXY 8-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
7. PAVEMENT MARKING STOPLINE EPOXY 12-INCH, WHITE
8. PAVEMENT MARKING STOPLINE EPOXY 18-INCH, WHITE
9. PAVEMENT MARKING ARROWS EPOXY, TYPE 3 WHITE
10. PAVEMENT MARKING WORDS BIKE LANE EPOXY, WHITE
11. PAVEMENT MARKING ARROWS BIKE LANE EPOXY, WHITE
12. PAVEMENT MARKING SYMBOLS BIKE LANE EPOXY, WHITE

2

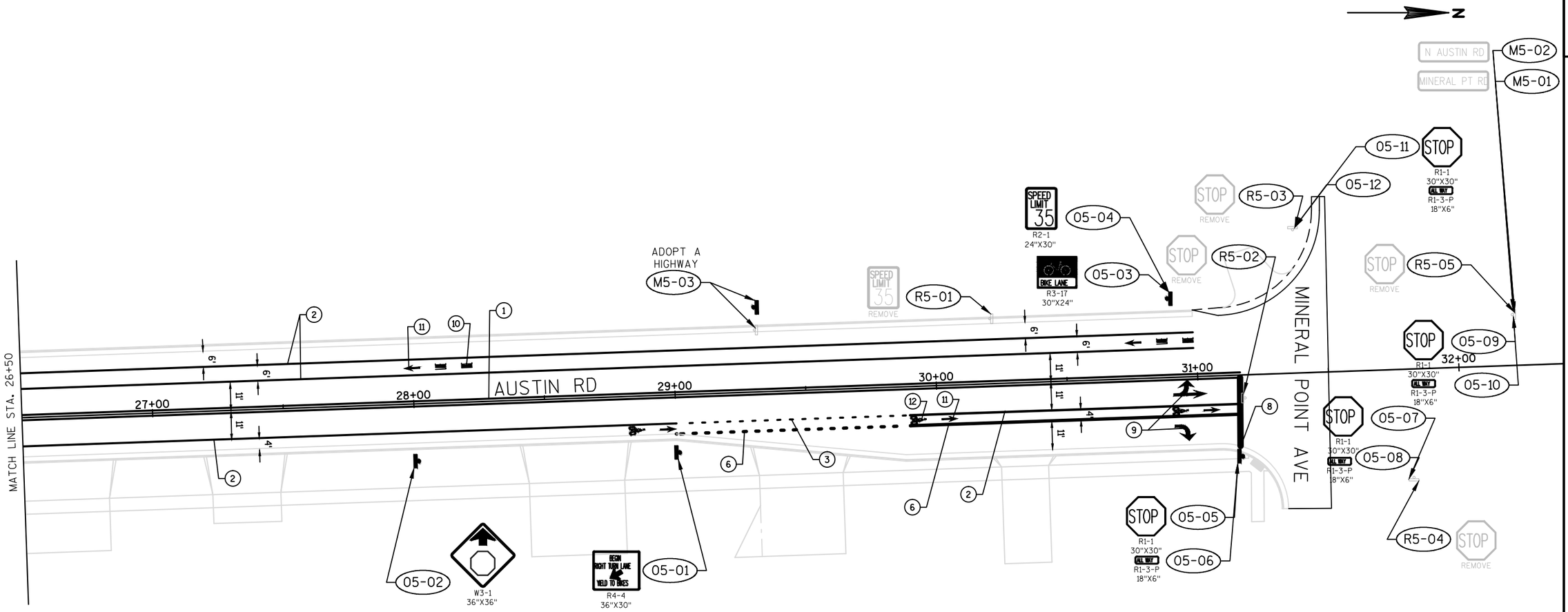


LEGEND

	EXISTING SIGN MOUNTED ON POST
	EXISTING SIGN MOUNTED ON POLE
	PROPOSED SIGN MOUNTED ON POST
	PROPOSED SIGN MOUNTED ON POLE
RX-XX	DENOTES SIGN TO BE REMOVED
MX-XX	DENOTES SIGN TO BE MOVED
XX-XX	DENOTES SIGN NUMBER

LEGEND

1. PAVEMENT MARKING EPOXY 4-INCH, DOUBLE YELLOW
2. PAVEMENT MARKING EPOXY 4-INCH, WHITE
3. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
4. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN YELLOW (3 FT LINE 9 FT SKIP)
5. PAVEMENT MARKING EPOXY 8-INCH, WHITE
6. PAVEMENT MARKING EPOXY 8-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
7. PAVEMENT MARKING STOPLINE EPOXY 12-INCH, WHITE
8. PAVEMENT MARKING STOPLINE EPOXY 18-INCH, WHITE
9. PAVEMENT MARKING ARROWS EPOXY, TYPE 3 WHITE
10. PAVEMENT MARKING WORDS BIKE LANE EPOXY, WHITE
11. PAVEMENT MARKING ARROWS BIKE LANE EPOXY, WHITE
12. PAVEMENT MARKING SYMBOLS BIKE LANE EPOXY, WHITE



LEGEND

	EXISTING SIGN MOUNTED ON POST
	EXISTING SIGN MOUNTED ON POLE
	PROPOSED SIGN MOUNTED ON POST
	PROPOSED SIGN MOUNTED ON POLE
RX-XX	DENOTES SIGN TO BE REMOVED
MX-XX	DENOTES SIGN TO BE MOVED
XX-XX	DENOTES SIGN NUMBER

LEGEND

1. PAVEMENT MARKING EPOXY 4-INCH, DOUBLE YELLOW
2. PAVEMENT MARKING EPOXY 4-INCH, WHITE
3. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
4. PAVEMENT MARKING EPOXY 4-INCH, DOT PATTERN YELLOW (3 FT LINE 9 FT SKIP)
5. PAVEMENT MARKING EPOXY 8-INCH, WHITE
6. PAVEMENT MARKING EPOXY 8-INCH, DOT PATTERN WHITE (3 FT LINE 9 FT SKIP)
7. PAVEMENT MARKING STOPLINE EPOXY 12-INCH, WHITE
8. PAVEMENT MARKING STOPLINE EPOXY 18-INCH, WHITE
9. PAVEMENT MARKING ARROWS EPOXY, TYPE 2 OR TYPE 3, WHITE
10. PAVEMENT MARKING WORDS BIKE LANE EPOXY, WHITE
11. PAVEMENT MARKING ARROWS BIKE LANE EPOXY, WHITE
12. PAVEMENT MARKING SYMBOLS BIKE LANE EPOXY, WHITE

GENERAL NOTES:

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. REMOVING/REPLACING OR COVERING/UNCOVERING SIGNS WILL BE INCIDENTAL TO OTHER TRAFFIC CONTROL ITEMS.

ALL TRAFFIC CONTROL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED IN PLANS.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS REFLECTIVE ORANGE.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS, ARROW BOARDS AND SIGN MESSAGE BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS, SIGN MESSAGE BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM OF 1500 FEET IN FRONT OF THE DRUMS.

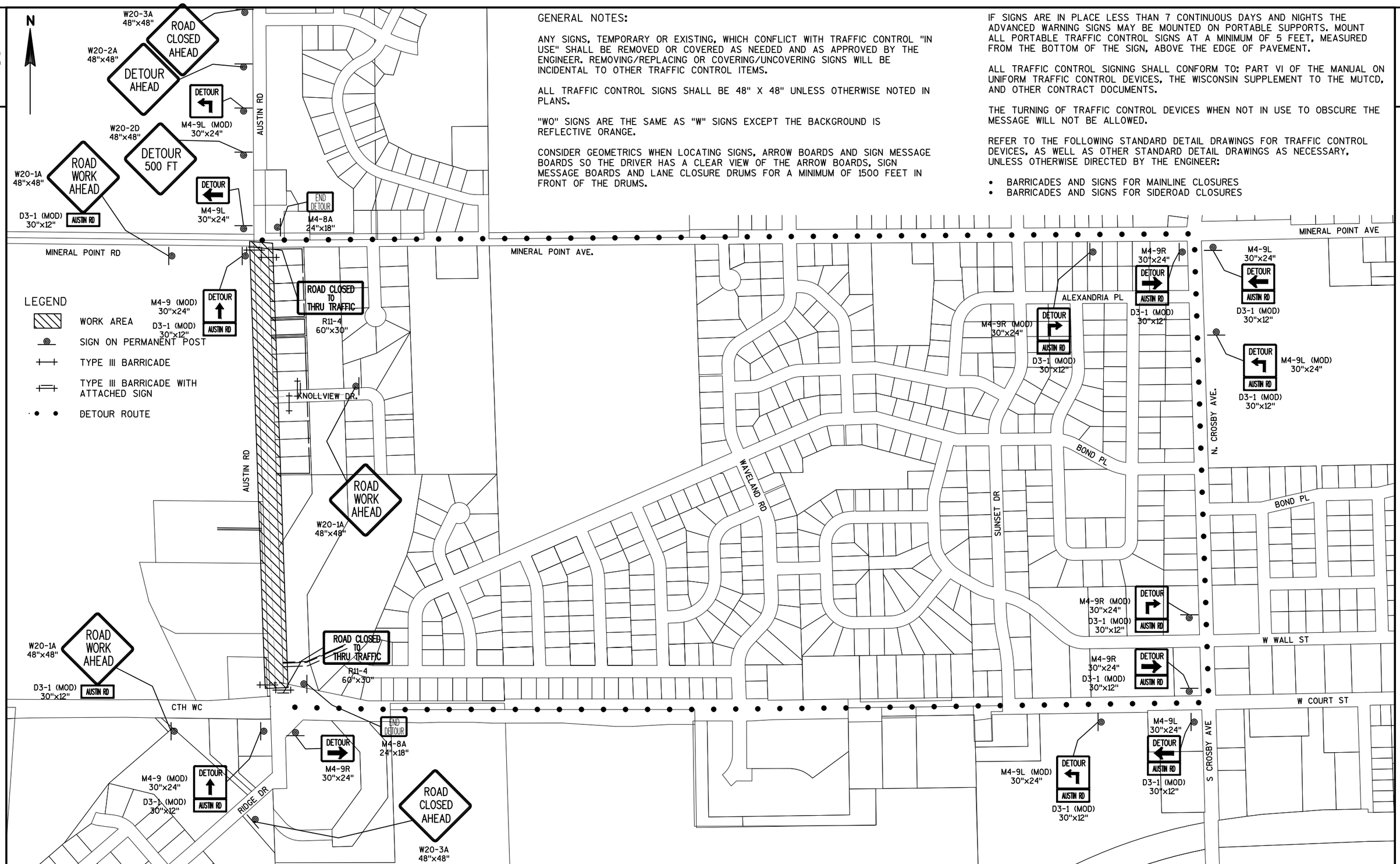
IF SIGNS ARE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS. MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.

ALL TRAFFIC CONTROL SIGNING SHALL CONFORM TO: PART VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, THE WISCONSIN SUPPLEMENT TO THE MUTCD, AND OTHER CONTRACT DOCUMENTS.

THE TURNING OF TRAFFIC CONTROL DEVICES WHEN NOT IN USE TO OBSCURE THE MESSAGE WILL NOT BE ALLOWED.

REFER TO THE FOLLOWING STANDARD DETAIL DRAWINGS FOR TRAFFIC CONTROL DEVICES, AS WELL AS OTHER STANDARD DETAIL DRAWINGS AS NECESSARY, UNLESS OTHERWISE DIRECTED BY THE ENGINEER:

- BARRICADES AND SIGNS FOR MAINLINE CLOSURES
- BARRICADES AND SIGNS FOR SIDEROAD CLOSURES



PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	TRAFFIC CONTROL/DETOUR PLAN	SHEET 38 E

2

BEGIN PROJECT ID. 5990-00-36
STA. 5+10

N

AUSTIN ROAD

6+00 8+00 N2° 00' 24"W 967.68' 10+00 12+00 14+00 N0° 51' 39"W 100.00' 16+00 18+00 N2° 00' 24"W 1732.32' 20+00

PI: 14+17.68

PI: 15+17.68

AUSTIN ROAD
B.P. = 5+10
X 480250.63
Y 267757.16

AUSTIN ROAD
P.I. = 14+17.68
X 480218.87
Y 268664.29

AUSTIN ROAD
P.I. = 15+17.68
X 480217.37
Y 268764.27

AUSTIN ROAD & KNOLLVIEW DRIVE
P.I. = 22+69.83, 100+00
X 480191.03
Y 269515.96

AUSTIN ROAD

18+00 N2° 00' 24"W 1732.32' 20+00 22+00 24+00 26+00 28+00 30+00 32+00

BEGIN CONSTRUCTION
STA. 100+00

KNOLL VIEW DR

N87° 59' 36"E 350.00'

102+00

END CONSTRUCTION
STA. 101+50

KNOLL VIEW DRIVE
E.P. = 101+50
X 480540.82
Y 269528.22

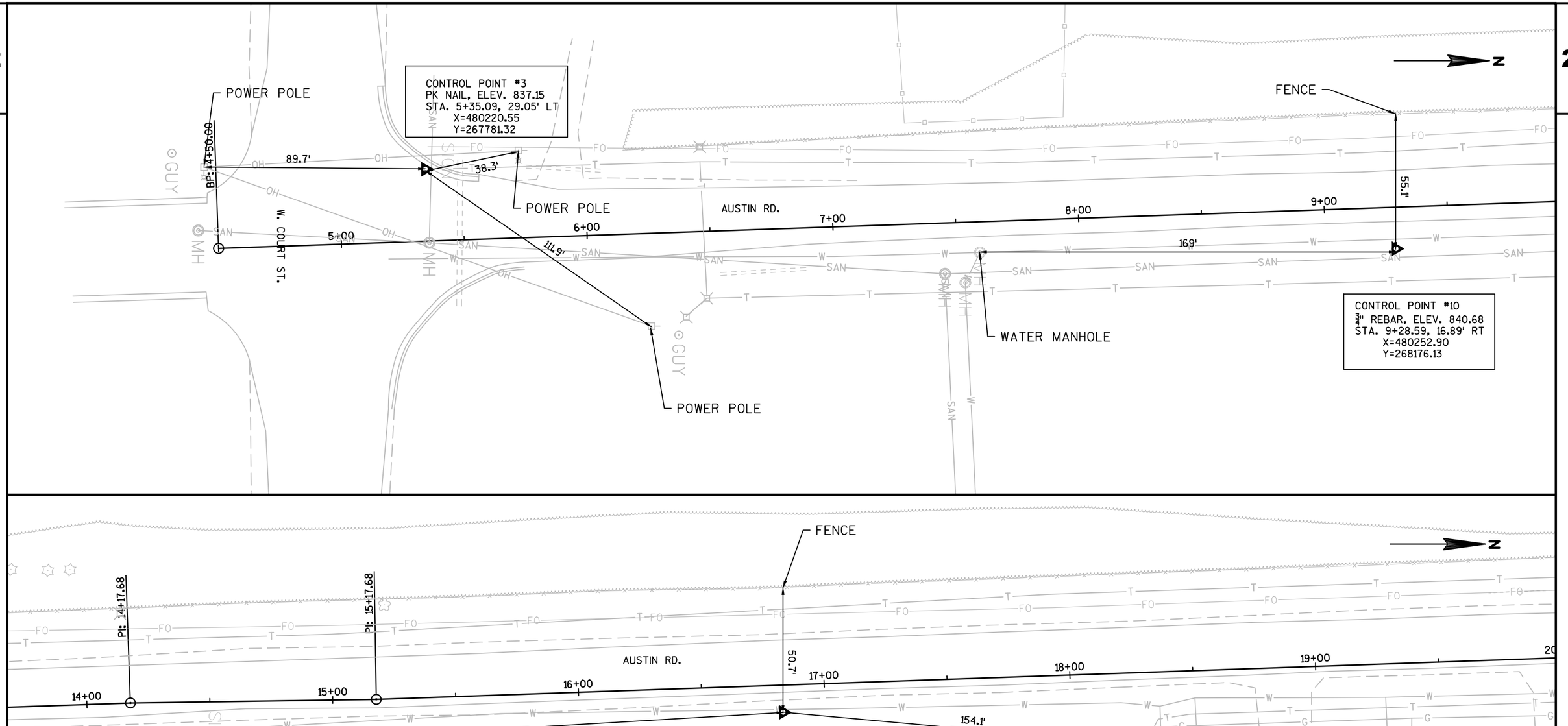
AUSTIN ROAD
E.P. = 31+50
X 480160.21
Y 270395.59

END PROJECT ID. 5990-00-36
STA. 31+50

N

PROJECT NO:5990-00-36	HWY:LOCAL ROAD	COUNTY:ROCK	ALIGNMENT DETAILS	SHEET 39 E

2



PROJECT NO:5990-00-36	HWY:LOCAL ROAD	COUNTY:ROCK	CONTROL POINT TIES	SHEET 40 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_027301_CP.DWG
LAYOUT NAME - 59900036_027301_CP - PLAN DBL 1 IN 40 FT
PLOT DATE : 7/21/2016 4:10 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

2

N

END OF CULVERT

CONTROL POINT #14
PK NAIL, ELEV. 872.52
STA. 22+47.71, 17.27' RT
X=480208.96
Y=269494.50

50.1'

BP: 100+00.00

54.3'

20'

STORM SEWER MANHOLE

HYDRANT

20+00 21+00 22+00 23+00 24+00 25+00

101+00 102+00 103+00

EP: 103+50.00

LIGHT POLE

286.6'

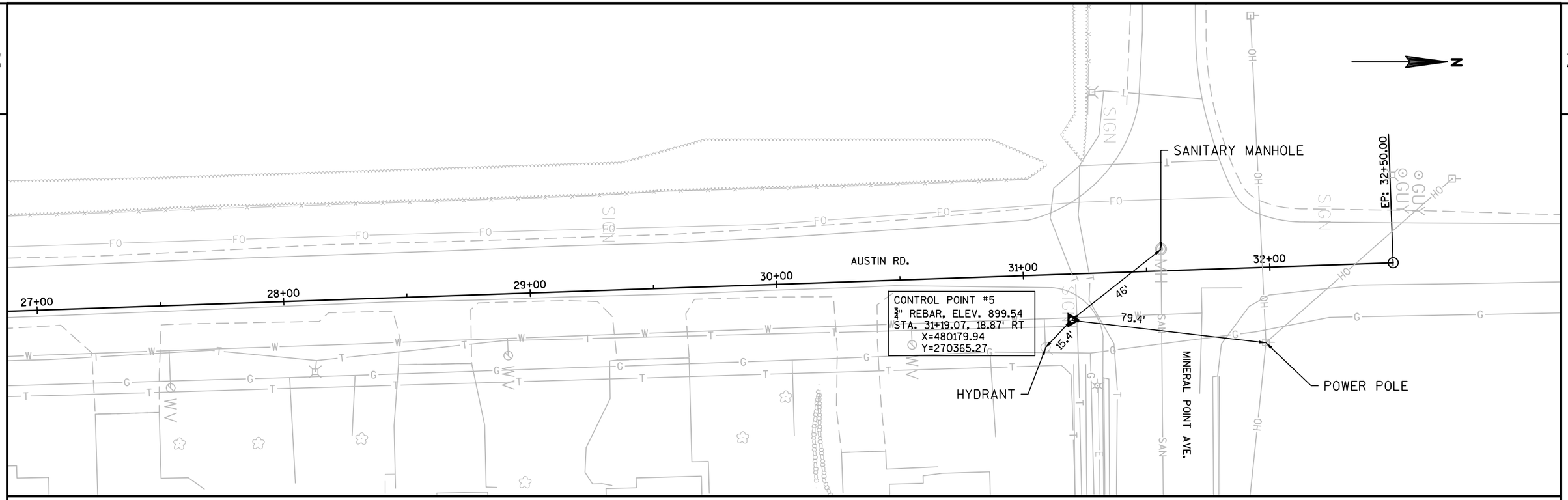
214.3'

SS MANHOLE

CONTROL POINT #7
MAG NAIL, ELEV. 868.74
STA. 22+59.91 AU, 622.25' RT
X=480813.18
Y=269528.07

PROJECT NO:5990-00-36	HWY:LOCAL ROAD	COUNTY:ROCK	CONTROL POINT TIES	SHEET 41 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_027301_CP.DWG
LAYOUT NAME - 59900036_027301_CP - PLAN DBL 1 IN 40 FT (2)
PLOT DATE : 7/21/2016 4:10 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT



2

PROJECT NO:5990-00-36	HWY:LOCAL ROAD	COUNTY:ROCK	CONTROL POINT TIES	SHEET 42 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_027301_CP.DWG
LAYOUT NAME - 59900036_027301_CP - PLAN DBL 1 IN 40 FT (3)
PLOT DATE : 7/21/2016 4:10 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

5990-00-36

Line	Item	Item Description	Unit	Total	Qty
0010	201.0105	Clearing	STA	24.000	24.000
0020	201.0205	Grubbing	STA	24.000	24.000
0030	203.0100	Removing Small Pipe Culverts	EACH	4.000	4.000
0040	203.0200	Removing Old Structure (station) 01. 11+77	LS	1.000	1.000
0050	204.0100	Removing Pavement	SY	569.000	569.000
0060	204.0150	Removing Curb & Gutter	LF	378.000	378.000
0070	204.0155	Removing Concrete Sidewalk	SY	10.000	10.000
0080	204.0170	Removing Fence	LF	2,436.000	2,436.000
0090	204.0195	Removing Concrete Bases	EACH	1.000	1.000
0100	204.0210	Removing Manholes	EACH	2.000	2.000
0110	204.0245	Removing Storm Sewer (size) 01. 12-inch	LF	158.000	158.000
0120	204.0280	Sealing Pipes	EACH	1.000	1.000
0130	205.0100	Excavation Common	CY	18,350.000	18,350.000
0140	213.0100	Finishing Roadway (project) 01. 5990-00-36	EACH	1.000	1.000
0150	305.0110	Base Aggregate Dense 3/4-Inch	TON	55.000	55.000
0160	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	11,500.000	11,500.000
0170	312.0110	Select Crushed Material	TON	13,000.000	13,000.000
0180	416.0160	Concrete Driveway 6-Inch	SY	1,350.000	1,350.000
0190	440.4410	Incentive IRI Ride	DOL	5,280.000	5,280.000
0200	455.0605	Tack Coat	GAL	650.000	650.000
0210	460.2000	Incentive Density HMA Pavement	DOL	2,240.000	2,240.000
0220	460.5223	HMA Pavement 3 LT 58-28 S	TON	2,000.000	2,000.000
0230	460.5225	HMA Pavement 5 LT 58-28 S	TON	1,500.000	1,500.000
0240	465.0105	Asphaltic Surface	TON	170.000	170.000
0250	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	20.000	20.000
0260	465.0315	Asphaltic Flumes	SY	30.000	30.000
0270	522.0112	Culvert Pipe Reinforced Concrete Class III 12-Inch	LF	32.000	32.000
0280	522.1012	Apron Endwalls for Culvert Pipe Reinforced Concrete 12-Inch	EACH	3.000	3.000
0290	522.1024	Apron Endwalls for Culvert Pipe Reinforced Concrete 24-Inch	EACH	2.000	2.000
0300	601.0419	Concrete Curb & Gutter 30-Inch Type L	LF	5,310.000	5,310.000
0310	601.0553	Concrete Curb & Gutter 4-Inch Sloped 36-Inch Type D	LF	150.000	150.000
0320	602.0405	Concrete Sidewalk 4-Inch	SF	7,000.000	7,000.000
0330	602.0415	Concrete Sidewalk 6-Inch	SF	250.000	250.000
0340	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	50.000	50.000
0350	606.0200	Riprap Medium	CY	25.000	25.000
0360	608.0318	Storm Sewer Pipe Reinforced Concrete Class III 18-Inch	LF	953.000	953.000
0370	608.0324	Storm Sewer Pipe Reinforced Concrete Class III 24-Inch	LF	442.000	442.000

5990-00-36

Line	Item	Item Description	Unit	Total	Qty
0380	608.0418	Storm Sewer Pipe Reinforced Concrete Class IV 18-Inch	LF	366.000	366.000
0390	608.0424	Storm Sewer Pipe Reinforced Concrete Class IV 24-Inch	LF	16.000	16.000
0400	608.0512	Storm Sewer Pipe Reinforced Concrete Class V 12-Inch	LF	238.000	238.000
0410	611.0420	Reconstructing Manholes	EACH	4.000	4.000
0420	611.0612	Inlet Covers Type C	EACH	1.000	1.000
0430	611.2004	Manholes 4-FT Diameter	EACH	9.000	9.000
0440	611.2005	Manholes 5-FT Diameter	EACH	2.000	2.000
0450	611.3230	Inlets 2x3-FT	EACH	10.000	10.000
0460	611.8110	Adjusting Manhole Covers	EACH	5.000	5.000
0470	611.8120.S	Cover Plates Temporary	EACH	5.000	5.000
0480	612.0902.S	Insulation Board Polystyrene (inch) 01. 2-inch	SY	10.000	10.000
0490	616.0700.S	Fence Safety	LF	100.000	100.000
0500	619.1000	Mobilization	EACH	1.000	1.000
0510	623.0200	Dust Control Surface Treatment	SY	15,000.000	15,000.000
0520	624.0100	Water	MGAL	600.000	600.000
0530	625.0500	Salvaged Topsoil	SY	20,000.000	20,000.000
0540	627.0200	Mulching	SY	10,600.000	10,600.000
0550	628.1504	Silt Fence	LF	4,000.000	4,000.000
0560	628.1520	Silt Fence Maintenance	LF	4,000.000	4,000.000
0570	628.1905	Mobilizations Erosion Control	EACH	5.000	5.000
0580	628.1910	Mobilizations Emergency Erosion Control	EACH	5.000	5.000
0590	628.2006	Erosion Mat Urban Class I Type A	SY	11,000.000	11,000.000
0600	628.7005	Inlet Protection Type A	EACH	25.000	25.000
0610	628.7010	Inlet Protection Type B	EACH	2.000	2.000
0620	628.7015	Inlet Protection Type C	EACH	15.000	15.000
0630	628.7555	Culvert Pipe Checks	EACH	5.000	5.000
0640	628.7560	Tracking Pads	EACH	2.000	2.000
0650	629.0210	Fertilizer Type B	CWT	15.000	15.000
0660	630.0140	Seeding Mixture No. 40	LB	400.000	400.000
0670	630.0200	Seeding Temporary	LB	600.000	600.000
0680	632.0101	Trees (species) (size) (root) 01. Turkish Filbert, B&B, 2 1/2" Cal.	EACH	9.000	9.000
0690	632.0101	Trees (species) (size) (root) 02. Hackberry, B&B, 2 1/2" Cal.	EACH	8.000	8.000
0700	632.0101	Trees (species) (size) (root) 03. Kentucky Coffee, B&B, 2" Cal.	EACH	3.000	3.000
0710	632.0101	Trees (species) (size) (root) 04. Honey Locust, B&B, 3" Cal.	EACH	3.000	3.000
0720	632.0101	Trees (species) (size) (root) 05. Hybrid Elm, B&B, 2"	EACH	6.000	6.000

5990-00-36

Line	Item	Item Description	Unit	Total	Qty
		Cal.			
0730	632.0101	Trees (species) (size) (root) 06. Tulip Tree, B&B, 3" Cal.	EACH	8.000	8.000
0740	632.0101	Trees (species) (size) (root) 07. Plane Tree, B&B, 2 1/2" Cal.	EACH	12.000	12.000
0750	632.0101	Trees (species) (size) (root) 08. Autumn Blaze Pear, B&B, 3" Cal.	EACH	4.000	4.000
0760	632.0101	Trees (species) (size) (root) 09. Swamp White Oak, B&B, 2" Cal.	EACH	6.000	6.000
0770	632.0101	Trees (species) (size) (root) 10. Bur Oak, B&B, 2 1/2" Cal.	EACH	6.000	6.000
0780	632.0101	Trees (species) (size) (root) 11. Regal Prince Oak, B&B, 2" Cal.	EACH	5.000	5.000
0790	632.9101	Landscape Planting Surveillance and Care Cycles	EACH	15.000	15.000
0800	633.5200	Markers Culvert End	EACH	4.000	4.000
0810	634.0810	Posts Tubular Steel 2x2-Inch X 10-FT	EACH	6.000	6.000
0820	634.0812	Posts Tubular Steel 2x2-Inch X 12-FT	EACH	6.000	6.000
0830	634.0814	Posts Tubular Steel 2x2-Inch X 14-FT	EACH	12.000	12.000
0840	637.2210	Signs Type II Reflective H	SF	116.000	116.000
0850	637.2230	Signs Type II Reflective F	SF	27.000	27.000
0860	638.2102	Moving Signs Type II	EACH	9.000	9.000
0870	638.2602	Removing Signs Type II	EACH	13.000	13.000
0880	638.3000	Removing Small Sign Supports	EACH	10.000	10.000
0890	642.5201	Field Office Type C	EACH	1.000	1.000
0900	643.0100	Traffic Control (project) 01. 5990-00-36	EACH	1.000	1.000
0910	643.0300	Traffic Control Drums	DAY	5,000.000	5,000.000
0920	643.0420	Traffic Control Barricades Type III	DAY	800.000	800.000
0930	643.0705	Traffic Control Warning Lights Type A	DAY	1,500.000	1,500.000
0940	643.0715	Traffic Control Warning Lights Type C	DAY	2,500.000	2,500.000
0950	643.0900	Traffic Control Signs	DAY	1,300.000	1,300.000
0960	643.2000	Traffic Control Detour (project) 01. 5990-00-36	EACH	1.000	1.000
0970	643.3000	Traffic Control Detour Signs	DAY	3,000.000	3,000.000
0980	645.0120	Geotextile Type HR	SY	40.000	40.000
0990	646.0106	Pavement Marking Epoxy 4-Inch	LF	12,000.000	12,000.000
1000	646.0126	Pavement Marking Epoxy 8-Inch	LF	160.000	160.000
1010	646.0600	Removing Pavement Markings	LF	160.000	160.000
1020	647.0166	Pavement Marking Arrows Epoxy Type 2	EACH	1.000	1.000
1030	647.0176	Pavement Marking Arrows Epoxy Type 3	EACH	1.000	1.000
1040	647.0206	Pavement Marking Arrows Bike Lane Epoxy	EACH	20.000	20.000
1050	647.0306	Pavement Marking Symbols Bike Lane Epoxy	EACH	3.000	3.000
1060	647.0406	Pavement Marking Words Bike Lane Epoxy	EACH	37.000	37.000
1070	647.0556	Pavement Marking Stop Line Epoxy 12-Inch	LF	5.000	5.000

5990-00-36

Line	Item	Item Description	Unit	Total	Qty
1080	647.0566	Pavement Marking Stop Line Epoxy 18-Inch	LF	90.000	90.000
1090	650.4000	Construction Staking Storm Sewer	EACH	26.000	26.000
1100	650.4500	Construction Staking Subgrade	LF	2,790.000	2,790.000
1110	650.5000	Construction Staking Base	LF	2,790.000	2,790.000
1120	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	5,460.000	5,460.000
1130	650.6000	Construction Staking Pipe Culverts	EACH	1.000	1.000
1140	650.8500	Construction Staking Electrical Installations (project) 01. 5990-00-36	LS	1.000	1.000
1150	650.9910	Construction Staking Supplemental Control (project) 01. 5990-00-36	LS	1.000	1.000
1160	650.9920	Construction Staking Slope Stakes	LF	2,790.000	2,790.000
1170	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	1,820.000	1,820.000
1180	653.0135	Pull Boxes Steel 24x36-Inch	EACH	2.000	2.000
1190	654.0105	Concrete Bases Type 5	EACH	5.000	5.000
1200	655.0610	Electrical Wire Lighting 12 AWG	LF	6,400.000	6,400.000
1210	655.0615	Electrical Wire Lighting 10 AWG	LF	300.000	300.000
1220	656.0200	Electrical Service Meter Breaker Pedestal (location) 01. Sta. 23+03	LS	1.000	1.000
1230	657.0255	Transformer Bases Breakaway 11 1/2-Inch Bolt Circle	EACH	5.000	5.000
1240	657.0322	Poles Type 5-Aluminum	EACH	5.000	5.000
1250	657.0615	Luminaire Arms Single Member 4 1/2-Inch Clamp 8-FT	EACH	5.000	5.000
1260	659.1125	Luminaires Utility LED C	EACH	5.000	5.000
1270	690.0150	Sawing Asphalt	LF	500.000	500.000
1280	690.0250	Sawing Concrete	LF	225.000	225.000
1290	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	300.000	300.000
1300	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	200.000	200.000
1310	SPV.0060	Special 01. Manhole Cover Special	EACH	10.000	10.000
1320	SPV.0060	Special 02. Inlet Cover Special	EACH	10.000	10.000
1330	SPV.0060	Special 03. Removing Street Light	EACH	1.000	1.000
1340	SPV.0060	Special 04. Mailbox Removal and Temporary Mailboxes	EACH	10.000	10.000
1350	SPV.0060	Special 05. Locate and Reference Property Corner	EACH	20.000	20.000
1360	SPV.0060	Special 06. Reset Property Corners	EACH	20.000	20.000
1370	SPV.0060	Special 07. Sanitary Manhole Chimney Seal	EACH	4.000	4.000
1380	SPV.0180	Special 01. Shredded Hardwood Bark Mulch	SY	140.000	140.000

3

CLEARING AND GRUBBING

	STATION	LOCATION	201.0105 CLEARING STA	201.0205 GRUBBING STA
CATEGORY 0010				
	6+00-23+00	Austin	17	17
	25+00-32+00	Austin	7	7
	TOTAL CATEGORY 0010		24	24

REMOVING CURB AND GUTTER

	STATION	LOCATION	204.0150 REMOVING CURB & GUTTER LF
CATEGORY 0010			
AUSTIN ROAD	5+19-5+74	LT/RT	147
	31+31	RT	9
KNOLL VIEW DR	100+37-101+50	LT/RT	222
	TOTAL CATEGORY 0010		378

REMOVING PAVEMENT

	STATION	LOCATION	204.0100 REMOVING PAVEMENT SY
CATEGORY 0010			
	18+77-18+97	RT	47
	19+88-20+08	RT	22
	20+40-20+81	RT	205
	26+42-26+82	RT	37
	28+43-28+80	RT	123
	29+21-29+63	RT	135
	TOTAL CATEGORY 0010		569

REMOVING CONCRETE SIDEWALK

	STATION	LOCATION	204.0155 REMOVING CONCRETE SIDEWALK SY
CATEGORY 0010			
	31+19-31+24	RT	10
	TOTAL CATEGORY 0010		10

REMOVING STORM SEWER

	STATION	LOCATION	203.0100 REMOVING SMALL PIPE CULVERTS EA	203.0200 REMOVING OLD STRUCTURE (11+77) LS	204.0210 REMOVING MANHOLES EA	204.0245.001 REMOVING STORM SEWER 12-INCH LF	204.0280 SEALING PIPES EACH
CATEGORY 0010							
AUSTIN ROAD	5+48	LT/RT	1	-	-	-	-
	5+90	LT	1	-	-	-	-
	6+71	RT	1	-	-	-	-
	11+77	LT/RT	-	1	-	-	-
	22+20	LT/RT	1	-	1	-	-
	22+34	RT	-	-	-	40	-
KNOLL VIEW DR	100+32	RT	-	-	1	-	-
	100+91	RT	-	-	-	118	1
	TOTAL CATEGORY 0010		4	1	2	158	1

REMOVING FENCING

	STATION	LOCATION	204.0170 REMOVING FENCE LF
CATEGORY 0020			
	5+10-31+50	LT	2436
	TOTAL CATEGORY 0020		2436

ASPHALT PAVEMENT ITEMS

	STATION	455.0605 TACK COAT GAL	460.5223 3 LT 58-28 S TON	460.5225 5 LT 58-28 S TON	465.0105 ASPHALTIC SURFACE TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD TON	465.0315 ASPHALTIC FLUMES SY
CATEGORY 0010							
Austin	5+10 - 31+50	552	1820	1365	160	10	21
Knoll View	100+00 - 101+50	26	58	43		-	-
	SUBTOTAL	578	1878	1408	160	10	21
	UNDISTRIBUTED	72	122	92	10	10	9
	TOTAL CATEGORY 0010	650	2000	1500	170	20	30

EXISTING MANHOLES

	STATION	LOCATION	611.0420 RECONSTRUCTING MANHOLES EA	611.8110 ADJUSTING MANHOLE COVERS EA	611.8120.S COVER PLATES TEMPORARY EA	SPV.0060.07 SANITARY MANHOLE CHIMNEY SEAL EA
CATEGORY 0020						
SANITARY SEWER	5+36	RT	-	1	1	1
	7+45	RT	-	1	1	1
	10+46	RT	1	-	-	1
	13+74	RT	1	-	-	1
WATER	7+53	RT	1	-	-	-
	7+59	RT	-	1	1	-
	22+82	RT	-	1	1	-
	UNDISTRIBUTED		1	1	1	-
	TOTAL CATEGORY 0020		4	5	5	4

BASE AGGREGATE ITEMS

	STATION		305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	312.0110 SELECT CRUSHED MATERIAL TON	623.0200 DUST CONTROL SY	624.0010 WATER MGAL
CATEGORY 0010							
Austin Road	5+10 - 31+50	LT/RT	25	10200	9000	13600	500
	5+89 - 6+01	LT	6				
	13+34 - 13+46	LT	14				
Knoll View Drive	100+00 - 101+50	LT/RT		225	200	600	20
	SUBTOTAL		45	10425	9200	14200	520
	UNDISTRIBUTED		10	1075	3800	800	80
	TOTAL CATEGORY 0010		55	11500	13000	15000	600

CONCRETE PAVEMENT ITEMS

	STATION		416.0160 CONCRETE DRIVEWAY 6-INCH SY
CATEGORY 0010			
	5+86 - 6+04	LT	30
	13+31 - 13+49	LT	35
	18+75 - 19+01	RT	97
	19+86 - 20+16	RT	90
	20+38 - 20+81	RT	86
	24+39 - 24+84	RT	126
	25+39 - 25+84	RT	128
	26+39 - 26+85	RT	160
	27+19 - 27+51	RT	78
	28+40 - 28+83	RT	177
	29+28 - 29+66	RT	155
	30+21 - 30+45	RT	82
KNOLL VIEW	101+18 - 101+43	LT	39
	SUBTOTAL		1283
	UNDISTRIBUTED		67
	TOTAL CATEGORY 0010		1350

CURB AND GUTTER

	STATION	LOCATION	601.0419 CONCRETE CURB & GUTTER 30-INCH TYPE L LF	601.0553 CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D LF
CATEGORY 0010				
Austin Road	5+10-5+80	LT/RT		150
	5+65-31+50	LT/RT	5100	
Knoll View Dr	100+00-101+50	LT/RT	210	
	TOTAL CATEGORY 0010		5310	150

CONCRETE SIDEWALK

	STATION	LOCATION	602.0405 CONCRETE SIDEWALK 4-INCH SF	602.0415 CONCRETE SIDEWALK 6-INCH SF	602.0515 CURB RAMP DETECTABLE WARNING FIELD NATURAL PATINA SF
CATEGORY 0010					
	5+21 - 5+31	RT	-	94	20
	5+89 - 6+01	LT	-	-	-
	13+34 - 13+46	LT	-	-	-
	14+18 - 18+78	RT	2301	-	-
	18+78 - 18+98	RT	-	-	-
	18+98 - 19+89	RT	455	-	-
	19+89 - 20+13	RT	-	-	-
	20+13 - 20+41	RT	140	-	-
	20+41 - 20+78	RT	-	-	-
	20+78 - 22+43	RT	824	-	-
	22+43 - 22+51	RT	-	47	10
	22+88 - 22+97	RT	-	47	10
	22+97 - 24+42	RT	727	-	-
	24+42 - 24+81	RT	-	-	-
	24+81 - 25+42	RT	303	-	-
	25+42 - 25+81	RT	-	-	-
	25+81 - 26+42	RT	306	-	-
	26+42 - 26+82	RT	-	-	-
	26+82 - 27+22	RT	198	-	-
	27+22 - 27+48	RT	-	-	-
	27+48 - 28+43	RT	475	-	-
	28+43 - 28+80	RT	-	-	-
	28+80 - 29+31	RT	258	-	-
	29+31 - 29+63	RT	-	-	-
	29+63 - 30+24	RT	303	-	-
	30+24 - 30+42	RT	-	-	-
	30+42 - 31+18	RT	383	-	-
	31+18 - 31+26	RT	-	31	-
	31+19 - 31+24	RT	91	-	10
KNOLL VIEW	101+18 - 101+43	LT	-	-	-
SUBTOTAL			6764	219	50
UNDISTRIBUTED			236	31	
TOTAL CATEGORY 0010			7000	250	50

STORM SEWER PIPE SUMMARY

	FROM STRUCTURE NO.		TO STRUCTURE NO.	INLET ELEV.	OUTLET ELEV.	SLOPE %	608.0318 STORM SEWER PIPE REINFORCED CONCRETE CLASS III 18-INCH LF	608.0324 STORM SEWER PIPE REINFORCED CONCRETE CLASS III 24-INCH LF	608.0418 STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 18-INCH LF	608.0424 STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 24-INCH LF	608.0512 STORM SEWER PIPE REINFORCED CONCRETE CLASS V 12-INCH LF	612.0902S INSULATION BOARD POLYSTYRENE 2-INCH SY
CATEGORY 0010												
	1-1.1	-	1-1.2	837.70	837.54	0.50%	-	-	-	-	32	-
	1-1.2	-	1-1.3	837.54	837.19	0.50%	-	-	-	-	70	-
	2-1.1	-	2-1.0	840.93	840.85	0.50%	-	-	16	-	-	-
	2-1.0	-	2-1.2	840.85	840.77	0.50%	-	-	-	16	-	-
	2-1.2	-	2-1.3	840.77	840.57	0.25%	-	80	-	-	-	-
	3-2.1	-	3-2.0	863.62	863.50	0.50%	-	-	-	-	24	-
	3-2.2	-	3-2.0	863.58	863.50	0.50%	-	-	-	-	16	-
	3-2.0	-	3-1.0	863.00	849.00	4.00%	350	-	-	-	-	-
	3-1.1	-	3-1.0	852.60	852.48	0.50%	-	-	-	-	24	-
	3-1.2	-	3-1.0	852.56	852.48	0.50%	-	-	-	-	16	-
	3-1.0	-	2-1.0	848.73	840.85	2.25%	-	-	350	-	-	-
	4-1.3	-	4-1.1	867.96	867.74	2.00%	11	-	-	-	-	-
	4-1.1	-	4-1.0	867.74	867.26	2.00%	24	-	-	-	-	-
	4-1.2	-	4-1.0	870.00	869.92	0.50%	-	-	-	-	16	-
	4-1.0	-	4-2.0	866.83	861.60	7.00%	-	75	-	-	-	-
	4-2.0	-	5-1.0	861.58	860.80	0.30%	-	260	-	-	-	4
	5-1.0	-	5-1.1	860.80	860.50	1.09%	-	27	-	-	-	-
	6-2.1	-	6-2.0	889.39	889.27	0.50%	-	-	-	-	24	-
	6-2.2	-	6-2.0	889.35	889.27	0.50%	-	-	-	-	16	-
	6-2.0	-	6-1.0	888.50	882.80	2.00%	285	-	-	-	-	-
	6-1.0	-	4-1.0	882.80	867.26	5.50%	283	-	-	-	-	-
SUBTOTAL							953	442	366	16	238	4
UNDISTRIBUTED							-	-	-	-	-	6
TOTAL CATEGORY 0010							953	442	366	16	238	10

CULVERT PIPE SUMMARY

	STATION	LOCATION	522.0112 CULVERT PIPE REINFORCED CONCRETE CLASS III 12-INCH LF
CATEGORY 0010			
	5+61	RT	32
TOTAL CATEGORY 0010			32

STORM SEWER STRUCTURE SUMMARY

	STRUCT. NO.	STATION	OFFSET	RIM ELEV	INLETS 611.3230 INLETS 2x3-FT EACH	MANHOLES 611.2004 MANHOLES 4-FT DIAMETER EACH	MANHOLES 611.2005 MANHOLES 5-FT DIAMETER EACH	611.0612 INLET COVERS TYPE C EACH	SPV.0060.01 MANHOLE COVER SPECIAL EACH	SPV.0060.02 INLET COVER SPECIAL EACH
CATEGORY 0010										
AUSTIN ROAD	1-1.1	9+00	16' LT	841.87	1	-	-	-	-	1
	1-1.2	9+00	16' RT	841.87	1	-	-	-	-	1
	2-1.0	12+50	0'	846.03	-	1	-	-	1	-
	2-1.1	12+50	16' LT	845.73	1	-	-	-	-	1
	2-1.2	12+50	16' RT	845.73	-	1	-	-	1	-
	3-1.0	16+00	0'	857.69	-	1	-	-	1	-
	3-1.1	16+00	24' LT	857.23	1	-	-	-	-	1
	3-1.2	16+00	16' RT	857.39	1	-	-	-	-	1
	3-2.0	19+50	0'	868.68	-	1	-	-	1	-
	3-2.1	19+50	24' LT	868.22	1	-	-	-	-	1
	3-2.2	19+50	16' RT	868.38	1	-	-	-	-	1
	4-2.0	22+43	0'	872.80	-	-	1	-	1	-
	4-1.0	23+18	0'	875.38	-	-	1	-	1	-
	4-1.1	23+18	24' LT	874.92	-	1	-	-	1	-
	4-1.2	23+18	16' RT	875.08	1	-	-	-	-	1
	4-1.3	23+18	35' LT	873.98	-	1	-	1	-	-
	6-1.0	26+00	0'	888.35	-	1	-	-	1	-
	6-2.0	28+85	0'	894.33	-	1	-	-	1	-
	6-2.1	28+85	24' LT	893.87	1	-	-	-	-	1
	6-2.2	28+85	16' RT	894.03	1	-	-	-	-	1
KNOLL VIEW DRIVE	5-1.0	102+60	23.5' RT	865.72	-	1	-	-	1	-
TOTAL CATEGORY 0010					10	9	2	1	10	10

3

STORM SEWER ENDWALL SUMMARY

	STATION	OFFSET	522.1012 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 12-INCH EACH	522.1024 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24-INCH EACH	616.0700.S FENCE SAFETY LF	633.5200 MARKERS CULVERT END EACH
CATEGORY 0010						
AUSTIN ROAD	5+61	LT/RT	2	-	-	1
	8+72	RT	1	-	-	1
	12+07	RT	-	1	-	1
KNOLL VIEW DR	102+70	RT	-	1	-	1
	103+00	RT	-	-	28	-
SUBTOTAL			3	2	28	4
UNDISTRIBUTED			-	-	72	-
TOTAL CATEGORY 0010			3	2	100	4

EROSION CONTROL ITEMS

	STATION	LOCATION	606.0200 RIPRAP MEDIUM CY	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.2006 EROSION MAT URBAN CLASS I TYPE A SY	628.7005 INLET PROTECTION TYPE A EA	628.7010 INLET PROTECTION TYPE B EA	628.7015 INLET PROTECTION TYPE C EA	628.7555 CULVERT PIPE CHECKS EA	628.7560 TRACKING PADS EA	645.0120 GEOTEXTILE FABRIC TYPE HR SY
CATEGORY 0010												
AUSTIN ROAD	5+10-31+50	LT/RT	14	3669	3669	10437	21	1	12	3	2	28
KNOLL VIEW	100+00-101+50	LT/RT	4	148	148	373	1	-	-	-	-	8
SUBTOTAL			18	3817	3817	10810	22	1	12	3	2	36
UNDISTRIBUTED			7	183	183	190	3	1	3	2	-	4
TOTAL CATEGORY 0010			25	4000	4000	11000	25	2	15	5	2	40

PAVEMENT MARKING

	STATION	LOCATION	646.0106 EPOXY 4-INCH WHITE LF	646.0106 EPOXY 4-INCH YELLOW LF	646.0126 EPOXY 8-INCH WHITE LF	646.0600 REMOVING PAVEMENT MARKING LF	647.0166 PAVEMENT MARKING ARROWS EPOXY TYPE 2 EA	647.0176 PAVEMENT MARKING ARROWS EPOXY TYPE 3 EA	647.0206 PAVEMENT MARKING ARROWS BIKE LANE EPOXY EA	647.0306 PAVEMENT MARKING SYMBOLS BIKE LANE EPOXY EA	647.0406 PAVEMENT MARKING WORDS BIKE LANE EPOXY EA	647.0556 STOP LINE EPOXY 12-INCH LF	647.0566 STOP LINE EPOXY 18-INCH LF
CATEGORY 0010													
	5+10-31+50	AUSTIN ROAD	6258	5393	148	-	1	1	20	3	37	5	77
		W COURT STREET	165	-	-	160	-	-	-	-	-	-	
		SUBTOTAL	6423	5393	148	160	1	1	20	3	37	5	77
	UNDISTRIBUTED		77	107	12	-	-	-	-	-	-	-	13
	TOTAL CATEGORY 0010		12000		160	160	1	1	20	3	37	5	90

CONSTRUCTION STAKING

	STATION	LOCATION	650.4000 CONSTRUCTION STAKING STORM SEWER EACH	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.5500 CONSTRUCTION STAKING CURB & GUTTER LF	650.6000 CONSTRUCTION STAKING PIPE CULVERTS EACH	650.8500 CONSTRUCTION STAKING ELECTRICAL INSTALLATIONS LS	650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL LS	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
CATEGORY 0010										
	5+10 - 31+50	AUSTIN	24	2640	2640	5250	1	1	1	2640
	100+00 - 101+50	KNOLL VIEW	2	150	150	210	-	-	-	150
SUBTOTAL			26	2790	2790	5460	1	1	1	2790
UNDISTRIBUTED			-	-	-	-	-	-	-	-
TOTAL CATEGORY 0010			26	2790	2790	5460	1	1	1	2790

SAWING PAVEMENT

	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF
CATEGORY 0010				
AUSTIN ROAD	5+10	LT/RT	276	-
	18+78-18+98	RT	-	20
	19+89-20+08	RT	-	20
	20+41-20+78	RT	-	38
	26+42-26+82	RT	-	41
	28+43-28+80	RT	-	37
	29+21-29+63	RT	-	42
	31+19-31+24	RT	-	5
	31+50	LT/RT	138	-
KNOLL VIEW DR	101+50	LT/RT	32	-
SUBTOTAL			446	203
UNDISTRIBUTED			54	22
TOTAL CATEGORY 0010			500	225

PERMANENT SIGNING

SIGN NO.	STATION	SIGN CODE	SIGN SIZE IN×IN	634.0810 POSTS TUBULAR STEEL 2×2-INCH x 10-FT EACH	634.0812 POSTS TUBULAR STEEL 2×2-INCH x 12-FT EACH	634.0814 POSTS TUBULAR STEEL 2×2-INCH x 14-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF	MOUNTED ON SAME POST AS
CATEGORY 0010									
1-01	5+54	R1-1	30x30	-	-	1	5.18	-	-
1-02	5+31	R1-1	18x18	1	-	-	1.86	-	-
1-03	5+33	R5-3	24x24	1	-	-	4	-	-
1-04	6+21	R2-1	24x30	-	-	1	5	-	-
1-05	6+21	R3-17	30x24	-	-	-	5	-	1-04
1-06	6+30	W3-1	18x18	1	-	-	-	2.25	-
1-07	7+82	W3-1	36x36	-	-	1	-	9	-
1-08	6+82	R3-17	30x24	-	1	-	5	-	-
1-09	6+82	R3-17B	30x12	-	-	-	2.5	-	1-08
1-10	8+82	R7-1D	18x24	-	1	-	3	-	-
1-11	8+21	R7-1D	18x24	-	1	-	3	-	-
2-01	14+51	R2-1	24x30	-	-	1	5	-	-
2-02	14+51	R7-1D	18x24	-	-	-	3	-	2-01
2-03	12+46	W1-3L	18x18	1	-	-	-	2.25	-
2-04	13+50	W5-4	18x18	1	-	-	-	2.25	-
2-05	15+17	W1-3L	18x18	1	-	-	-	2.25	-
2-06	15+17	R7-1L	18x24	-	1	-	3	-	-
3-01	19+09	R2-1	24x30	-	-	1	5	-	-
3-02	19+10	R7-1D	18x24	-	-	1	3	-	-
4-01	22+92	R1-1	30x30	-	-	1	5.18	-	-
4-02	22+92	R7-2L	18x24	-	1	-	3	-	-
4-03	23+40	R7-1D	18x24	-	-	1	3	-	-
4-04	23+40	R3-17	30x24	-	-	-	5	-	4-03
4-05	26+33	R2-1	24x30	-	-	1	5	-	-
5-01	29+00	R4-4	36x30	-	-	1	7.5	-	-
5-02	28+00	W3-1	36x36	-	-	1	-	9	-
5-03	30+90	R3-17	30x24	-	-	1	5	-	5-04
5-04	30+90	R2-1	24x30	-	-	-	5	-	-
5-05	31+16	R1-1	30x30	-	1	-	5.18	-	-
5-06	31+16	R1-3P	18x6	-	-	-	0.75	-	5-05
5-07	31+81	R1-1	30x30	-	-	-	5.18	-	-
5-08	31+81	R1-3P	18x6	-	-	-	0.75	-	5-07
5-09	32+21	R1-1	30x30	-	-	-	5.18	-	-
5-10	32+21	R1-3P	18x6	-	-	-	0.75	-	5-09
5-11	31+38	R1-1	30x30	-	-	-	5.18	-	-
5-12	31+38	R1-3P	18x6	-	-	-	0.75	-	5-11
TOTAL CATEGORY 0010				6	6	12	116	27	

REMOVING SIGNS

SIGN NO.	SIGN CODE	638.2102 MOVING SIGNS TYPE II EA	638.2602 REMOVING SIGNS TYPE II EA	638.3000 REMOVING SMALL SIGN SUPPORTS EA	SIGN MOUNTED ON SAME POST AS	MESSAGE
CATEGORY 0010						
R1-01	R1-1	-	1	1	-	STOP SIGN
R1-02	R2-1	-	1	1	-	SPEED LIMIT 35
M1-01	-	1	-	-	1-01	STREET NAME SIGN
M1-02	-	1	-	-	1-01	STREET NAME SIGN
M1-03	-	1	-	-	-	ADOPT A HIGHWAY
R2-01	R2-1	-	1	1	-	SPEED LIMIT 35
R3-01	R2-1	-	1	1	-	SPEED LIMIT 35
R3-02	W3-1	-	1	1	-	STOP AHEAD
R4-01	W3-1	-	1	1	-	STOP AHEAD
R4-02	R1-1	-	1	1	-	STOP SIGN
R4-03	R2-1	-	1	1	-	SPEED LIMIT 35
M4-01	W14-2	1	-	-	-	NO OUTLET
M4-02	-	1	-	-	4-01	STREET NAME SIGN
M4-03	-	1	-	-	4-01	STREET NAME SIGN
R5-01	R2-1	-	1	1	-	SPEED LIMIT 35
R5-02	R1-1	-	1	1	-	STOP SIGN
R5-03	R1-1	-	1	-	-	STOP SIGN
R5-04	R1-1	-	1	-	-	STOP SIGN
R5-05	R1-1	-	1	-	-	STOP SIGN
M5-01	-	1	-	-	5-09	STREET NAME SIGN
M5-02	-	1	-	-	5-09	STREET NAME SIGN
M5-03	-	1	-	-	-	ADOPT A HIGHWAY
TOTAL CATEGORY 0010		9	13	10		

LANDSCAPING ITEMS

LOCATION	625.0500 SALVAGED TOPSOIL SY	627.0200 MULCHING SY	629.0210 FERTILIZER TYPE B CWT	630.0140 SEEDING MIXTURE NO. 40 LB	630.0200 SEEDING TEMPORARY LB	632.0101.01 TREES TURKISH FILBERT, B&B, 2.5" CAL. EACH	632.0101.02 TREES HACKBERRY, B&B, 2.5" CAL. EACH	632.0101.03 TREES KENTUCKY COFFEE TREE, B&B, 2" CAL. EACH	632.0101.04 TREES HONEY LOCUST, B&B, 3" CAL. EACH	632.0101.05 TREES HYBRID ELM, B&B, 2" CAL. EACH	632.0101.06 TREES TULIP TREE, B&B, 3" CAL. EACH	632.0101.07 TREES PLANE TREE, B&B, 2.5" CAL. EACH	632.0101.08 TREES AUTUMN BLAZE PEAR, B&B, 3" CAL. EACH	632.0101.09 TREES SWAMP WHITE OAK, B&B, 2" CAL. EACH	632.0101.10 TREES BUR OAK, B&B, 2.5" CAL. EACH	632.0101.11 TREES REGAL PRINCE OAK, B&B, 2" CAL. EACH	632.9101 LANDSCAPE PLANTING SURVEILLANCE AND CARE CYCLES EACH	SPV.0180.01 SHREDDED HARDWOOD BARK MULCH SY
CATEGORY 0010																		
STA. 5+10-31+50	18625	7976	12	335	503	9	8	3	3	6	8	12	4	6	6	5	15	140
SUBTOTAL	18625	7976	12	335	503	9	8	3	3	6	8	12	4	6	6	5	15	140
UNDISTRIBUTED	1375	2624	3	65	97	-	-	-	-	-	-	-	-	-	-	-	-	-
TOTAL CATEGORY 0010	20000	10600	15	400	600	9	8	3	3	6	8	12	4	6	6	5	15	140

LIGHTING ITEMS

STATION	LOCATION	204.0195 REMOVING CONCRETE BASES EACH	653.0135 PULL BOXES STEEL 24x36-INCH EA	654.0105 CONCRETE BASES TYPE 5 EACH	656.0200 ELECTRICAL SERVICE METER BREAKER PEDESTAL STA.23+03 LS	657.0255 TRANSFORMER BASES BREAKAWAY 11 1/2-INCH BOLT CIRCLE EACH	657.0322 POLES TYPE 5-ALUMINUM EACH	657.0615 LUMINAIRE ARMS SINGLE MEMBER 4 1/2-INCH CLAMP 8-FT EACH	659.1125 LUMINAIRES UTILITY LED C EACH	SPV.0060.03 REMOVING STREET LIGHT EACH
CATEGORY 0010										
9+50	22.5' RT	-	-	1	-	1	1	1	1	-
14+00	22.5' RT	-	-	1	-	1	1	1	1	-
18+25	22.5' RT	-	-	1	-	1	1	1	1	-
22+48	54' RT	-	1	-	-	-	-	-	-	-
22+90	37.7' RT	1	-	-	-	-	-	-	-	1
22+98	54' RT	-	1	-	-	-	-	-	-	-
23+00	29.1' RT	-	-	1	-	1	1	1	1	-
23+03	54' RT	-	-	-	1	-	-	-	-	-
27+00	22.5' RT	-	-	1	-	1	1	1	1	-
TOTAL CATEGORY 0010		1	2	5	1	5	5	5	5	1

TRAFFIC CONTROL ITEMS

STATION	643.0300 TRAFFIC CONTROL DRUMS DAYS	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAYS	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAYS	643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C DAYS	643.0900 TRAFFIC CONTROL SIGNS DAYS	643.3000 TRAFFIC CONTROL DETOUR SIGNS DAYS
CATEGORY 0010						
AUSTIN ROAD	5000	540	1080	2500	900	2430
SUBTOTAL	5000	540	1080	2500	900	2430
UNDISTRIBUTED	-	260	420	-	400	570
TOTAL CATEGORY 0010	5000	800	1500	2500	1300	3000

EARTHWORK SUMMARY

Division	From/To Station	Common Excavation (1) Cut (2)	(item # 205.0100) EBS Excavation (3)	Salvaged/Unusable Pavement Material (4)	Available Material (5)	Unexpanded Fill	Expanded Fill (6) Factor 1.25	Mass Ordinate +/- (7)	Waste	Comment:
AUSTIN ROAD	5+10 - 31+50	16,559	1,600	611	15,948	6,029	7,536	8,412	10,012	
KNOLLVIEW DRIVE	100+50 - 101+50	166	25	27	139	351	439	-300	0	
Grand Total		16,725	1,625	638	16,087	6,380	7,975	8,112	10,012	
	Total Common Exc		18,350							

1) Common Excavation is the sum of the Cut and EBS Excavation columns. Item number 205.0100
2) Salvaged/Unsuable Pavement Material is included in Cut.
3) EBS Excavation includes topsoil removal from fill areas (assumed 1' depth) within the roadway limits.
4) Salvaged/Unusable Pavement Material
5) Available Material = Cut - Salvaged/Unusuable Pavement Material
6) Expanded Fill. Factor = 1.25
   Or Expanded Fill = Unexpanded Fill * Fill Factor
7) The Mass Ordinate + or - Qty calculated for the Division. Plus quantity indicates an excess of material within the Division. Minus indicates a shortage of material within the Division.

WIRING ITEMS

LOCATION	652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH LF	655.0610 ELECTRICAL WIRE LIGHTING 12 AWG LF	655.0615 ELECTRICAL WIRE LIGHTING 10 AWG LF
CATEGORY 0010			
LP1	-	150	-
LP1-LP2	456	1410	-
LP2	-	150	-
LP2-LP3	427	1335	-
LP3	-	150	-
LP3-LPB1	449	1329	-
LPB1-LPB2	50	-	210
LPB2-ES1	5	-	75
ES1-LPB2	-	75	-
LP4	-	150	
LPB2-LP4	25	135	-
LP5	-	150	
LP4-LP5	408	1305	-
TOTAL CATEGORY 0010	1820	6339	285
UNDISTRIBUTED	-	61	15
TOTAL CATEGORY 0010	1820	6400	300

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTICTIVE DEVELOPMENT EASEMENT	RDE
CENTERLINE	C/L	RIGHT	RT
CERTIFIED SURVEY MAP	CSM	RIGHT OF WAY	R/W
CONCRETE	CONC	SECTION	SEC
COUNTY	CO	SEPTIC VENT	SEPV
COUNTY TRUNK HIGHWAY	CTH	SQUARE FEET	SF
DISTANCE	DIST	STATE TRUNK HIGHWAY	STH
CORNER	COR	STATION	STA
DOCUMENT NUMBER	DOC	TELEPHONE PEDESTAL	TP
EASEMENT	EASE	TEMPORARY LIMITED EASEMENT	TLE
EXISTING	EX	TRANSPORTATION PROJECT PLAT	TPP
GAS VALVE	GV	UNITED STATES HIGHWAY	USH
GRID NORTH	GN	VOLUME	V
HIGHWAY EASEMENT	HE		
IDENTIFICATION	ID		
LAND CONTRACT	LC		
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

WATER	W
GAS	G
TELEPHONE	T
OVERHEAD TRANSMISSION LINES	OH
ELECTRIC	E
CABLE TELEVISION	TV
FIBER OPTIC	FO
SANITARY SEWER	SAN
STORM SEWER	SS

CONVENTIONAL SYMBOLS

SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W OR HE LINE
PROPERTY LINE
LOT, TIE & OTHER MINOR LINES
SLOPE INTERCEPT
CORPORATE LIMITS
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)
TEMPORARY LIMITED EASEMENT AREA
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)
TRANSMISSION STRUCTURES
BUILDING — TO BE REMOVED
BRIDGE

SECTION CORNER SYMBOL
SECTION CORNER MONUMENT
R/W MONUMENT (TO BE SET)
NON-MONUMENTED R/W POINT
FOUND IRON PIN (1-INCH UNLESS NOTED)
GEODETIC SURVEY MONUMENT
SIXTEENTH CORNER MONUMENT
SIGN
OFF-PREMISE SIGN

	COMPENSABLE	NON-COMPENSABLE
ELECTRIC POLE		
TELEPHONE POLE		
PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		

ACCESS RESTRICTED BY ACQUISITION
NO ACCESS (BY STATUTORY AUTHORITY)
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)
NO ACCESS (NEW HIGHWAY)
PARCEL NUMBER 25 — UTILITY NUMBER 40
PARALLEL OFFSETS

CAUTION
THIS PLAT IS FOR ILLUSTRATIVE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES.

R/W PROJECT NUMBER 5990-00-35	SHEET NUMBER	TOTAL SHEETS
CONSTRUCTION PROJECT NUMBER 5990-00-36	4.01	8

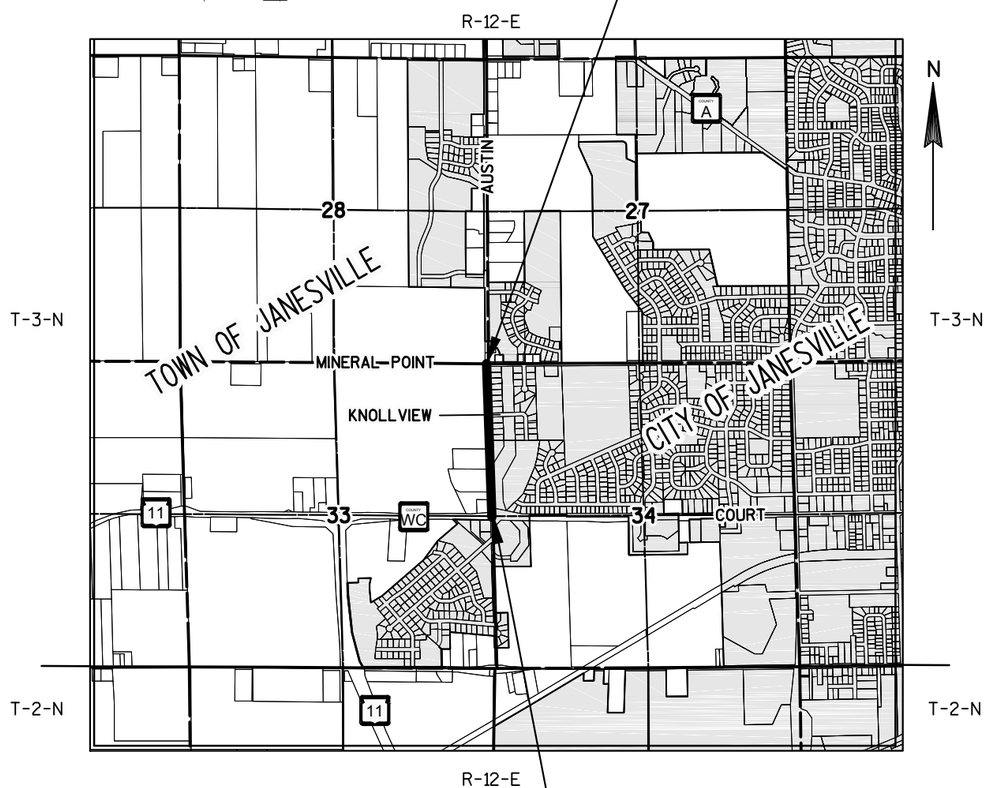
PLAT OF RIGHT OF WAY REQUIRED FOR

CITY OF JANESVILLE, AUSTIN ROAD

(WEST COURT STREET TO MINERAL POINT AVENUE)

LOCAL STREET — ROCK COUNTY

END RELOCATION ORDER
STATION 31+50.00
42.35' E OF AND 9.28' S
OF THE N.E. COR. OF SEC. 33, T-3-N, R-12-E
AND THE N.W. COR. OF SEC. 34, T-3-N, R-12-E



LAYOUT
SCALE 0 — 0.5 MILE

TOTAL NET LENGTH OF CENTERLINE = 0.5 MI.

BEGIN RELOCATION ORDER
STATION 5+10.00
39.63' E OF AND 11.39' N
OF THE E. CORNER OF SEC. 33, T-3-N, R-12-E
AND THE W. CORNER OF SEC. 34, T-3-N, R-12-E

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, ROCK COUNTY, NAD 83 (2011) IN US SURVEY FEET, VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY 3/4" X 24" REBAR) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

ALL RIGHT-OF-WAY LINES DEPICTED IN NON-ACQUISTION AREAS ARE INTENDED TO REESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENT.

PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS AND INTERESTS REQUIRED.

DIMENSIONING FOR THE RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO NEW REFERENCE LINES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: EXISTING HIGHWAY RIGHT-OF-WAY FOR AUSTIN ROAD ESTABLISHED FROM PREVIOUS PROJECTS INGOLD'S KNOLL FIRST ADDITION SUBDIVISION, AUSTIN ROAD CONDOMINIUM I, INGOLD'S KNOLL SUBDIVISION, CSM DOC. NO. 946448 (VOL 10, PGS 439-440), CSM DOC. NO. 1992722 (VOL 36, PGS 43-47), CSM DOC. NO. 1590069 (VOL 26, PGS 15-17) AND HIGHWAY RECORDS FOR THE TOWN OF JANESVILLE (PG 304, MAY 28, 1859).

A **TEMPORARY LIMITED EASEMENT** (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE OR PLANT THEREON VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. ALL TLES EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

ACCEPTED FOR
CITY OF JANESVILLE

2-3-16 (Date) — (DIRECTOR OF PUBLIC WORKS)

ORIGINAL PLAT PREPARED BY

Batterman
engineers surveyors planners

R.H. BATTERMAN & CO., INC.
2857 BARTELLS DRIVE
BELOIT, WI 53511
P 608.365.4464
TF 877.457.2235
F 608.365.1850

WISCONSIN
JEFFREY R. GARDE
S-2766
BRODHEAD
WIS.
LAND SURVEYOR

THIS SURVEY IS PREPARED AT THE REQUEST OF THE CITY. THE FIELD SURVEY WAS PERFORMED IN NOVEMBER 2014. THIS SURVEY IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

(SIGNATURE) Jeffrey R Garde
DATE 2/1/2016
(PRINTED NAME) Jeffrey R. Garde
(REGISTRATION NUMBER) S-2766 52

REVISION DATE
2/18/2016

SCHEDULE OF LANDS & INTERESTS REQUIRED

AREAS SHOWN IN THE TOTAL AREA COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM TAX ROLLS OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE ACQUIRED.

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO THE CITY.

PARCEL NUMBER	SHEET NUMBER	OWNER(S)	INTEREST REQUIRED	R/W ACRES OR SQUARE FEET REQUIRED			P.L.E. ACRES	T.L.E. ACRES
				NEW	EXISTING	TOTAL		
1	4.04	HOWARD L & ELIZABETH F AUSTIN	T.L.E.	---	---	---	---	0.07
2	4.04 - 4.06	HOWARD L AUSTIN	T.L.E.	---	---	---	---	0.35
3	4.04, 4.05, 4.07	CITY OF JANESVILLE	T.L.E.	---	---	---	---	0.94
4	4.05, 4.06	MARKLEIN BUILDERS INC	T.L.E.	---	---	---	---	0.19
6	4.06 - 4.08	F&C AUSTIN LAND CORP	T.L.E.	---	---	---	---	0.74
7	4.06	LINDA M WEIS	T.L.E.	---	---	---	---	0.06
8	4.06	TIMOTHY W & LEIGH E LAMPSON	T.L.E.	---	---	---	---	0.03
9	4.06, 4.07	CYNTHIA A VISGAR	T.L.E.	---	---	---	---	0.03
10 (1)	4.07	MICHAEL J & CHARLENE REIS WITTLIEFF	T.L.E.	---	---	---	---	0.09
11	4.07	ARTHUR & NANCY WIESE JARSTAD	T.L.E.	---	---	---	---	0.19
12 (1)	4.07	KURT D MARLING	T.L.E.	---	---	---	---	0.06
13	4.07, 4.08	RIDGEWAY REALTY, LLC	T.L.E.	---	---	---	---	0.04
14	4.08	JAMES D JR & BRENDA L NOLTE	T.L.E.	---	---	---	---	0.03
16	4.08	MICHAEL E & MARY JEAN MILLER	T.L.E.	---	---	---	---	0.03
17	4.08	WILLIAM A & SUZANNE C RENTMEESTER	T.L.E.	---	---	---	---	0.03
18	4.08	NATHANIEL W & LISA M SIMMONS	T.L.E.	---	---	---	---	0.07
19	4.08	CHRISTINE R FISHER	T.L.E.	---	---	---	---	0.06
21	4.04, 4.05	CITY OF JANESVILLE	TEMPORARY RELEASE OF RIGHTS	---	---	---	---	---
22	4.08	AT&T OF WISCONSIN	TEMPORARY RELEASE OF RIGHTS	---	---	---	---	---

4

4

REVISION DATE 2/18/2016

DATE 2/1/16

GRID FACTOR _____

SCALE. FEET
0 -- --

HWY: AUSTIN ROAD

COUNTY: ROCK

STATE R/W PROJECT NUMBER 5990-00-35

CONSTRUCTION PROJECT NUMBER 5990-00-36

PLAT SHEET 4.02

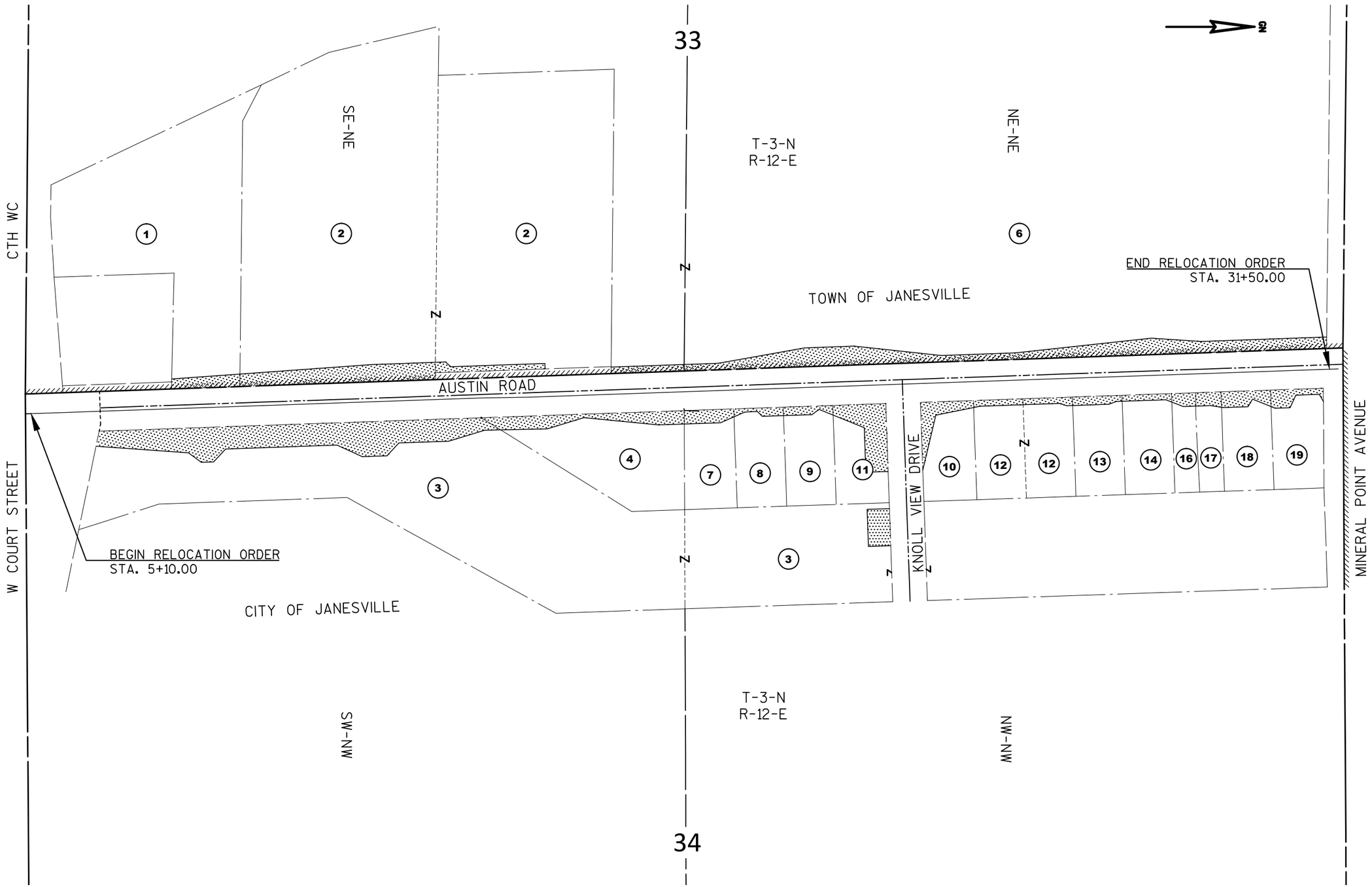
PS&E SHEET 53

E

FILE NAME : 32006-RW PLAT.DWG
LAYOUT NAME - ****

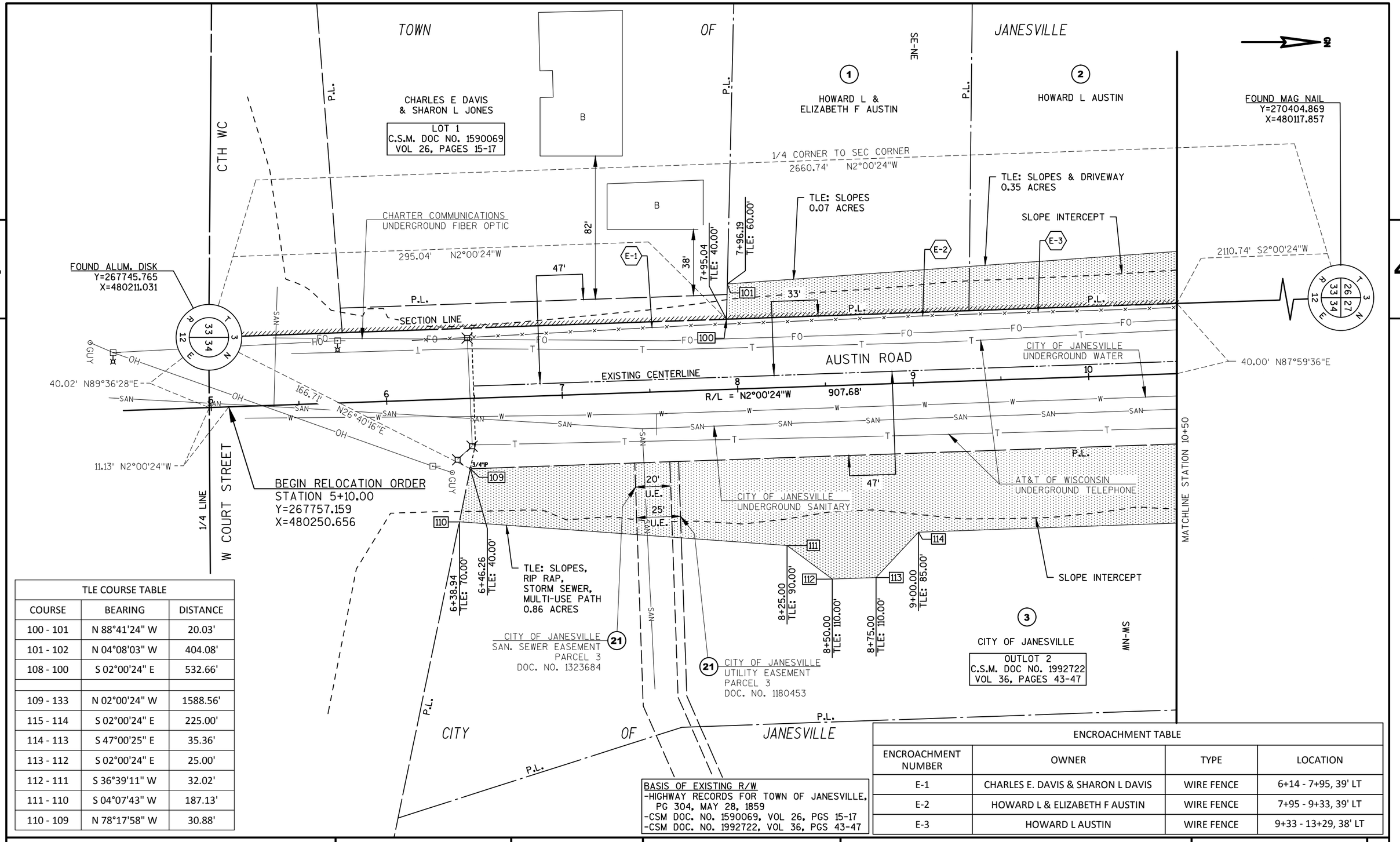
PLOT DATE : 2/19/2016 10:21 AM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : **********

WISDOT/CADDS SHEET 75



4

REVISION DATE				DATE 2/1/16	SCALE, FEET 0 100 200	HWY: AUSTIN ROAD	STATE R/W PROJECT NUMBER 5990-00-35	PLAT SHEET 4.03	
2/18/2016				GRID FACTOR _____		COUNTY: ROCK	CONSTRUCTION PROJECT NUMBER 5990-00-36	PS&E SHEET 54	E



TLE COURSE TABLE		
COURSE	BEARING	DISTANCE
100 - 101	N 88°41'24" W	20.03'
101 - 102	N 04°08'03" W	404.08'
108 - 100	S 02°00'24" E	532.66'
109 - 133	N 02°00'24" W	1588.56'
115 - 114	S 02°00'24" E	225.00'
114 - 113	S 47°00'25" E	35.36'
113 - 112	S 02°00'24" E	25.00'
112 - 111	S 36°39'11" W	32.02'
111 - 110	S 04°07'43" W	187.13'
110 - 109	N 78°17'58" W	30.88'

ENCROACHMENT TABLE			
ENCROACHMENT NUMBER	OWNER	TYPE	LOCATION
E-1	CHARLES E. DAVIS & SHARON L DAVIS	WIRE FENCE	6+14 - 7+95, 39' LT
E-2	HOWARD L & ELIZABETH F AUSTIN	WIRE FENCE	7+95 - 9+33, 39' LT
E-3	HOWARD L AUSTIN	WIRE FENCE	9+33 - 13+29, 38' LT

REVISION DATE			
2/18/2016(NC)			

DATE 2/1/16

GRID FACTOR _____

SCALE, FEET 0 25 50

HWY: AUSTIN ROAD

COUNTY: ROCK

STATE R/W PROJECT NUMBER 5990-00-35

CONSTRUCTION PROJECT NUMBER 5990-00-36

PLAT SHEET 4.04

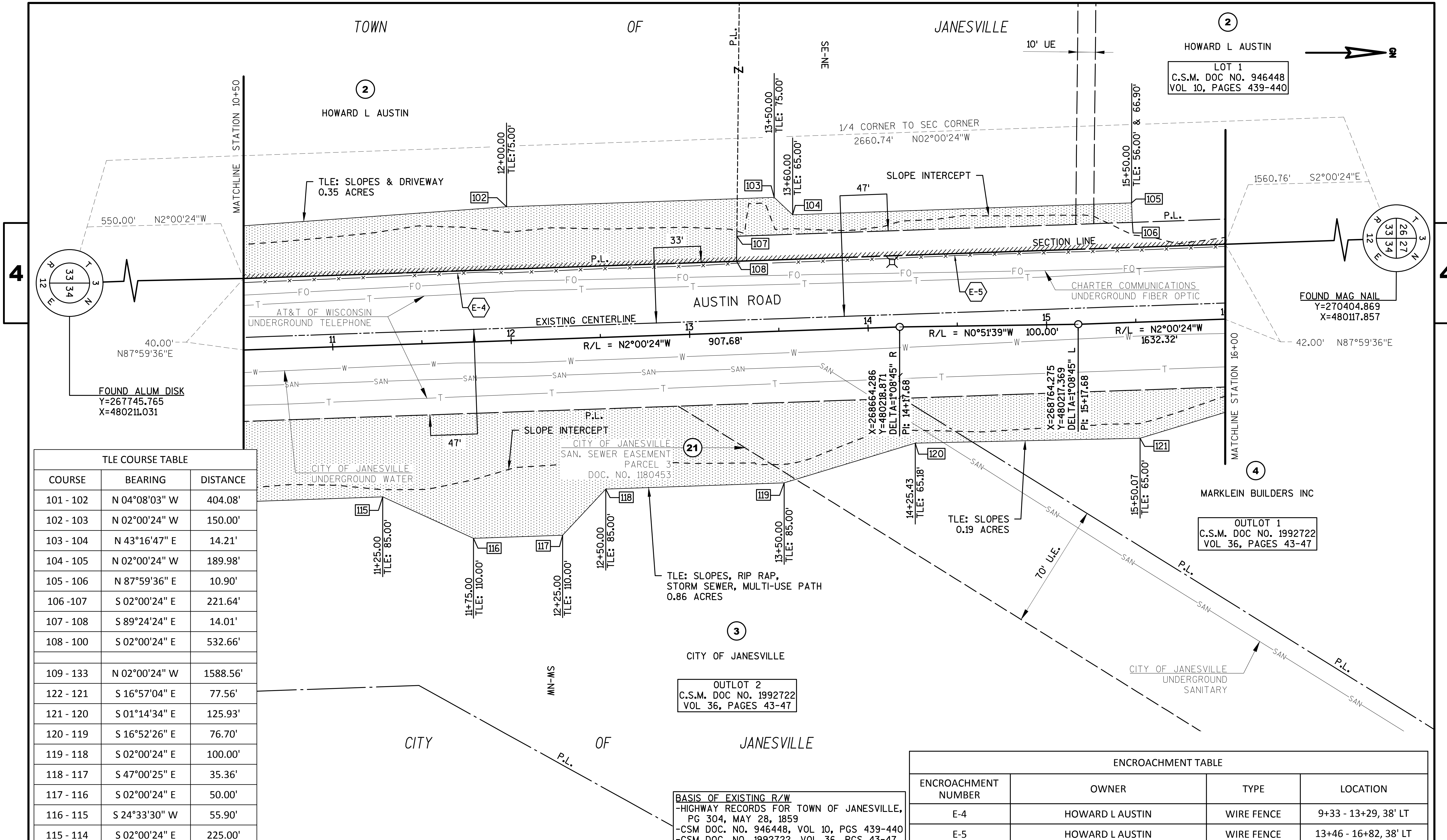
PS&E SHEET 55

E

FILE NAME : 32006-RW PLAT.DWG
LAYOUT NAME - ****

PLOT DATE : 2/18/2016 11:11 AM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 75



TOWN OF JANESVILLE
HOWARD L AUSTIN
LOT 1
C.S.M. DOC NO. 946448
VOL 10, PAGES 439-440
10' UE
P.L.
SE-NE
MATCHLINE STATION 10+50
TLE: SLOPES & DRIVEWAY
0.35 ACRES
12+00.00
TLE:75.00'
13+50.00
TLE: 75.00'
13+60.00
TLE: 65.00'
15+50.00
TLE: 56.00' & 66.90'
1/4 CORNER TO SEC CORNER
2660.74' N02°00'24"W
SLOPE INTERCEPT
1560.76' S2°00'24"E
550.00' N2°00'24"W
SECTION LINE
AUSTIN ROAD
EXISTING CENTERLINE
AT&T OF WISCONSIN
UNDERGROUND TELEPHONE
CHARTER COMMUNICATIONS
UNDERGROUND FIBER OPTIC
E-4
E-5
R/L = N2°00'24"W 907.68'
R/L = N0°51'39"W 100.00'
R/L = N2°00'24"W 1632.32'
40.00'
N87°59'36"E
42.00' N87°59'36"E
FOUND ALUM DISK
Y=267745.765
X=480211.031
FOUND MAG NAIL
Y=270404.869
X=480117.857
X=268664.286
Y=480218.871
DELTA=1°08'45" R
PI: 14+17.68
X=268764.275
Y=480217.369
DELTA=1°08'45" L
PI: 15+17.68
MATCHLINE STATION 16+00
MARKLEIN BUILDERS INC
OUTLOT 1
C.S.M. DOC NO. 1992722
VOL 36, PAGES 43-47
CITY OF JANESVILLE
SAN. SEWER EASEMENT
PARCEL 3
DOC. NO. 1180453
CITY OF JANESVILLE
UNDERGROUND WATER
11+25.00
TLE: 85.00'
11+75.00
TLE: 110.00'
12+25.00
TLE: 110.00'
12+50.00
TLE: 85.00'
13+50.00
TLE: 85.00'
14+25.43
TLE: 65.18'
15+50.07
TLE: 65.00'
TLE: SLOPES
0.19 ACRES
TLE: SLOPES, RIP RAP,
STORM SEWER, MULTI-USE PATH
0.86 ACRES
70' U.E.
CITY OF JANESVILLE
OUTLOT 2
C.S.M. DOC NO. 1992722
VOL 36, PAGES 43-47
SW-NW
CITY OF JANESVILLE
UNDERGROUND SANITARY
CITY OF JANESVILLE

TLE COURSE TABLE

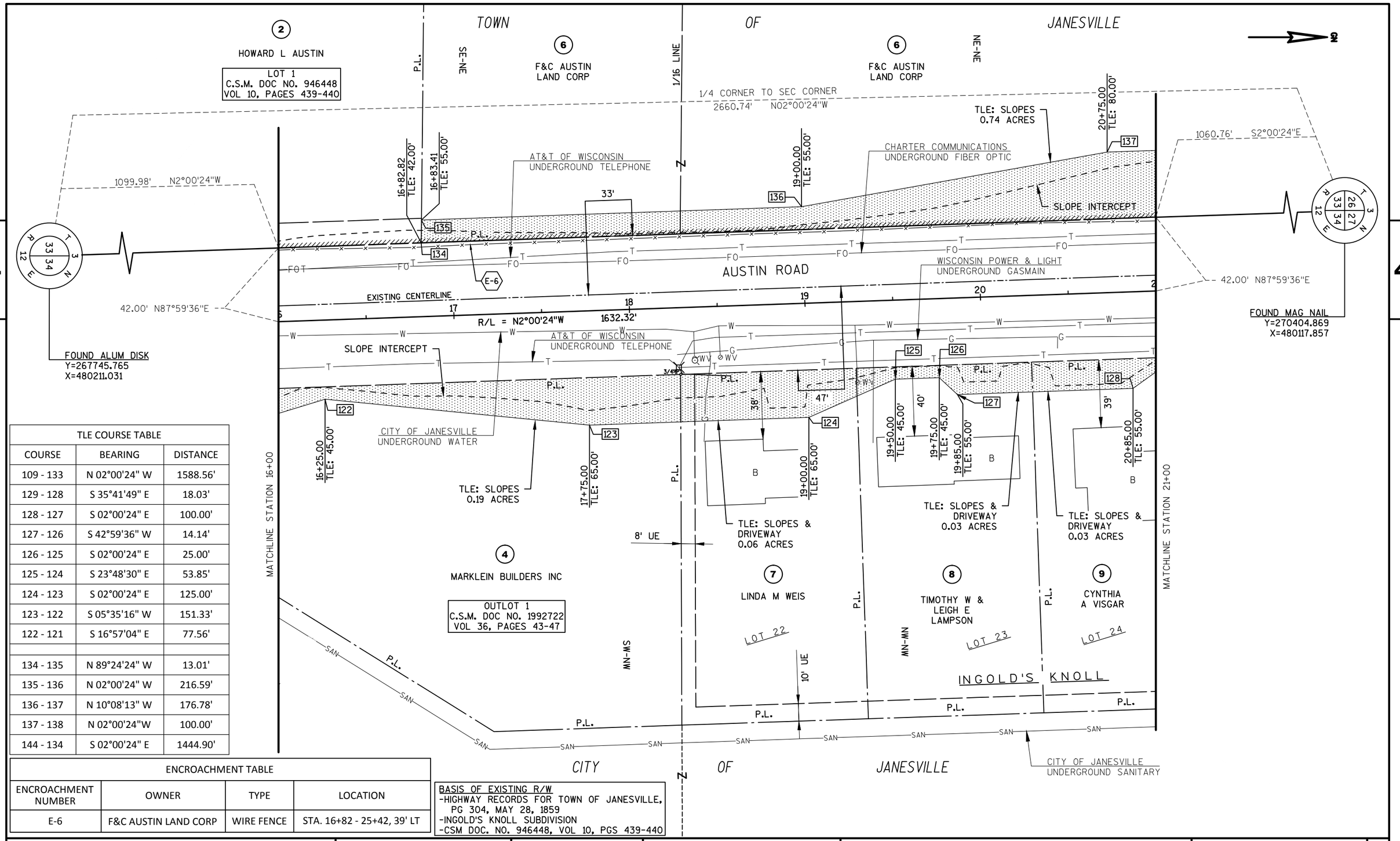
COURSE	BEARING	DISTANCE
101 - 102	N 04°08'03" W	404.08'
102 - 103	N 02°00'24" W	150.00'
103 - 104	N 43°16'47" E	14.21'
104 - 105	N 02°00'24" W	189.98'
105 - 106	N 87°59'36" E	10.90'
106 -107	S 02°00'24" E	221.64'
107 - 108	S 89°24'24" E	14.01'
108 - 100	S 02°00'24" E	532.66'
109 - 133	N 02°00'24" W	1588.56'
122 - 121	S 16°57'04" E	77.56'
121 - 120	S 01°14'34" E	125.93'
120 - 119	S 16°52'26" E	76.70'
119 - 118	S 02°00'24" E	100.00'
118 - 117	S 47°00'25" E	35.36'
117 - 116	S 02°00'24" E	50.00'
116 - 115	S 24°33'30" W	55.90'
115 - 114	S 02°00'24" E	225.00'

BASIS OF EXISTING R/W
- HIGHWAY RECORDS FOR TOWN OF JANESVILLE, PG 304, MAY 28, 1859
- CSM DOC. NO. 946448, VOL 10, PGS 439-440
- CSM DOC. NO. 1992722, VOL 36, PGS 43-47

ENCROACHMENT TABLE

ENCROACHMENT NUMBER	OWNER	TYPE	LOCATION
E-4	HOWARD L AUSTIN	WIRE FENCE	9+33 - 13+29, 38' LT
E-5	HOWARD L AUSTIN	WIRE FENCE	13+46 - 16+82, 38' LT

REVISION DATE 2/18/2016(NC)	DATE 2/1/16	SCALE, FEET 0 25 50	HWY: AUSTIN ROAD	STATE R/W PROJECT NUMBER 5990-00-35	PLAT SHEET 4.05
	GRID FACTOR		COUNTY: ROCK	CONSTRUCTION PROJECT NUMBER 5990-00-36	PS&E SHEET 56 E



TLE COURSE TABLE		
COURSE	BEARING	DISTANCE
109 - 133	N 02°00'24" W	1588.56'
129 - 128	S 35°41'49" E	18.03'
128 - 127	S 02°00'24" E	100.00'
127 - 126	S 42°59'36" W	14.14'
126 - 125	S 02°00'24" E	25.00'
125 - 124	S 23°48'30" E	53.85'
124 - 123	S 02°00'24" E	125.00'
123 - 122	S 05°35'16" W	151.33'
122 - 121	S 16°57'04" E	77.56'
134 - 135	N 89°24'24" W	13.01'
135 - 136	N 02°00'24" W	216.59'
136 - 137	N 10°08'13" W	176.78'
137 - 138	N 02°00'24"W	100.00'
144 - 134	S 02°00'24" E	1444.90'

ENCROACHMENT TABLE			
ENCROACHMENT NUMBER	OWNER	TYPE	LOCATION
E-6	F&C AUSTIN LAND CORP	WIRE FENCE	STA. 16+82 - 25+42, 39' LT

BASIS OF EXISTING R/W
-HIGHWAY RECORDS FOR TOWN OF JANESVILLE, PG 304, MAY 28, 1859
-INGOLD'S KNOLL SUBDIVISION
-CSM DOC. NO. 946448, VOL 10, PGS 439-440

REVISION DATE 2/18/2016(NC)	DATE 2/1/16	SCALE, FEET 0 25 50	HWY: AUSTIN ROAD
GRID FACTOR _____			COUNTY: ROCK

STATE R/W PROJECT NUMBER 5990-00-35	PLAT SHEET 4.06
CONSTRUCTION PROJECT NUMBER 5990-00-36	PS&E SHEET 57 E

FILE NAME : 32006-RW PLAT.DWG
LAYOUT NAME - ****
PLOT DATE : 2/18/2016 11:12 AM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 75

TOWN OF JANESVILLE

FOUND ALUM DISK
Y=267745.765
X=480211.031

1599.98' N02°00'24"W

42.00' N87°59'36"E

STATION EQUATION:
22+69.83 = 100+00.00
Y=269515.963
X=480191.030

21+75.00 TLE: 80.00'

23+50.00 TLE: 55.00'

25+00.00 TLE: 55.00'

1/4 CORNER TO SEC CORNER
2660.74' N02°00'24"W

510.76' S02°00'24"E

42.00' N87°59'36"E

FOUND MAG NAIL
Y=270404.869
X=480117.857

F&C AUSTIN LAND CORP

TLE: SLOPES 0.74 ACRES

SLOPE INTERCEPT

CHARTER COMMUNICATIONS
UNDERGROUND FIBER OPTIC

AT&T OF WISCONSIN
UNDERGROUND TELEPHONE

CITY OF JANESVILLE
UNDERGROUND WATER

AUSTIN ROAD

R/L = N2°00'24"W 1632.32'

SECTION LINE

TLE: SLOPES & DRIVEWAY 0.03 ACRES

TLE: SLOPES & DRIVEWAY 0.04 ACRES

WISCONSIN POWER & LIGHT
UNDERGROUND GASMAIN

21+00.00 TLE: 45.00'

100+85.00 TLE: 80.00'

TLE: SLOPES 0.19 ACRES

23+34.83 TLE: 65.00'

TLE: SLOPES 0.09 ACRES

SLOPE INTERCEPT

24+25.00 TLE: 50.00'

25+85.00 TLE: 50.00'

26+00.00 TLE: 55.00'

TLE: SLOPES & DRIVEWAY 0.06 ACRES

MATCHLINE STATION 21+00

MATCHLINE STATION 26+50

R/L = N87°59'36"E

KNOLL VIEW DRIVE

9 CYNTHIA A VISGAR

11 ARTHUR & NANCY WIESE JARSTAD

10 MICHAEL J & CHARLENE REIS WITTLIEFF

12 KURT D MARLING

12 KURT D MARLING

13 RIDGEWAY REALTY, LLC

3 CITY OF JANESVILLE

INGOLD'S KNOLL

INGOLD'S KNOLL

INGOLD'S KNOLL FIRST ADDITION

LOT 24 LOT 25 LOT 26 LOT 27 LOT 28 LOT 29

8' UE

10' UE

WISCONSIN POWER & LIGHT
UNDERGROUND ELECTRIC

AT&T OF WISCONSIN
UNDERGROUND TELEPHONE

CITY OF JANESVILLE
UNDERGROUND SANITARY

TLE: STORM SEWER & RIP RAP 0.08 ACRES

WISCONSIN POWER & LIGHT
UNDERGROUND ELECTRIC

CITY OF JANESVILLE
UNDERGROUND STORM

101+74.92 TLE: 80.00'

101+74.97 TLE: 35.00'

101+75.03 TLE: 35.00'

102+50.00 TLE: 35.00' & 80.00'

103+25.00 TLE: 35.00' & 80.00'

CITY OF JANESVILLE

MN-MN

TLE COURSE TABLE		
COURSE	BEARING	DISTANCE
137 - 138	N 02°00'24" W	100.00'
138 - 139	N 06°07'24" E	176.78'
139 - 140	N 02°00'24" W	150.00'
140 - 141	N 06°09'59" W	275.73'
144 - 134	S 02°00'24" E	1444.90'
109 - 133	N 02°00'24" W	1588.56'
133 - 132	N 87°59'36" E	139.96'
132 - 145	N 87°59'36" E	75.03'
145 - 148	N 87°59'36" E	75.00'
148 - 147	S 02°00'24" E	45.00'
147 - 146	S 87°59'36" W	75.00'
146 - 145	N 02°00'24" W	45.00'
145 - 132	S 87°59'36" W	75.03'
132 - 131	S 01°57'00" E	45.00'
131 - 130	S 87°59'36" W	89.92'
130 - 129	S 21°59'43" W	98.34'
129 - 128	S 35°41'49" E	18.03'
155 - 154	S 02°00'24" E	85.00'
154 - 153	S 16°25'41" W	15.81'
153 - 152	S 02°00'24" E	160.00'
152 - 151	S 11°27'07" E	91.41'
151 - 150	S 76°45'22" E	114.05'
150 - 149	S 87°59'36" W	137.03'
149 - 168	N 02°00'24" W	813.64'

BASIS OF EXISTING R/W
-HIGHWAY RECORDS FOR TOWN OF JANESVILLE, PG 304, MAY 28, 1859
-INGOLD'S KNOLL SUBDIVISION
-INGOLD'S KNOLL FIRST ADDITION SUBDIVISION

ENCROACHMENT TABLE			
ENCROACHMENT NUMBER	OWNER	TYPE	LOCATION
E-7	F&C AUSTIN LAND CORP	WIRE FENCE	STA. 16+82 - 25+42, 39' LT
E-8	F&C AUSTIN LAND CORP	WIRE FENCE	STA. 25+75 - 31+03, 39' LT

REVISION DATE 2/18/2016

DATE 2/1/16

GRID FACTOR _____

SCALE, FEET 0 25 50

HWY: AUSTIN ROAD

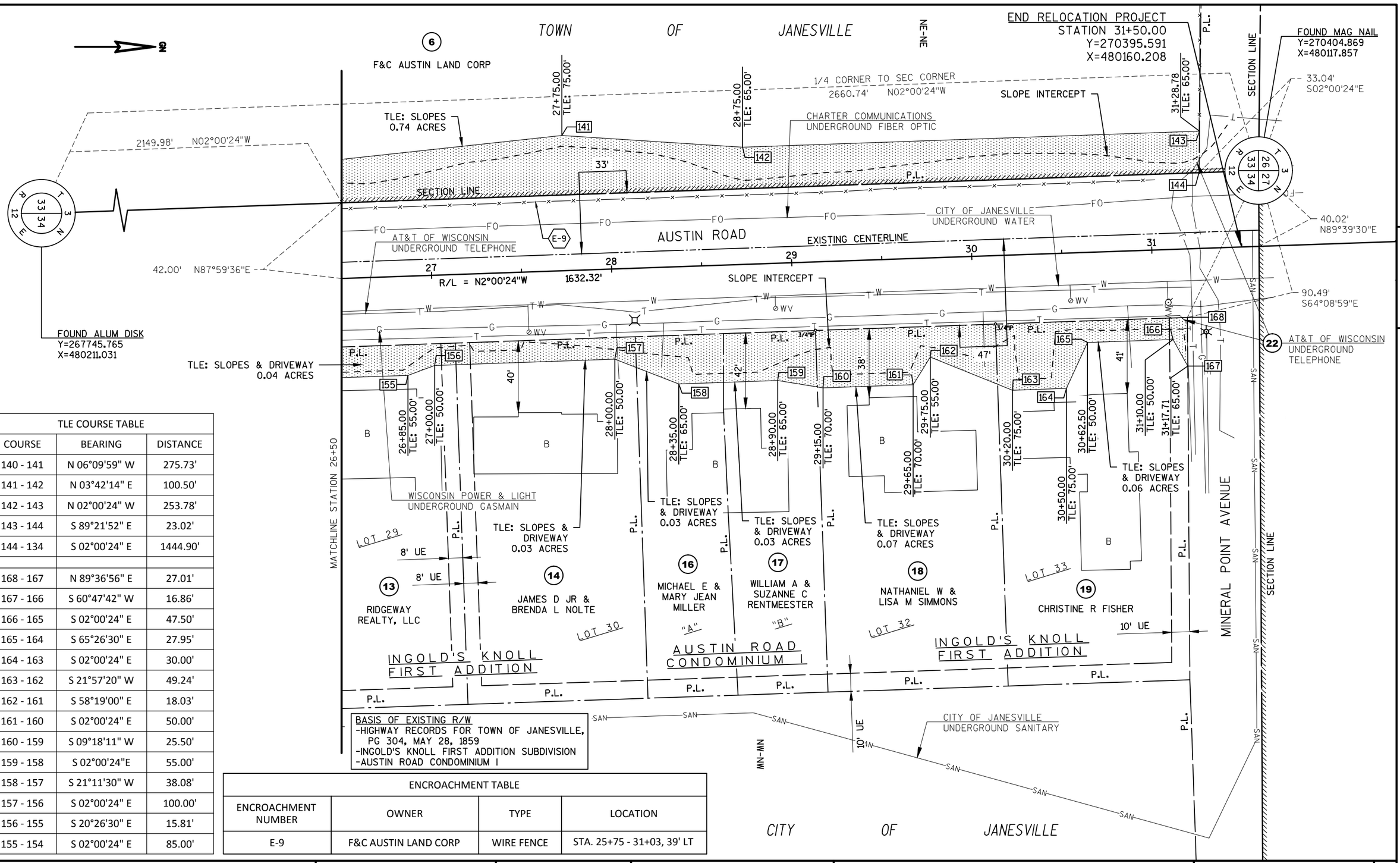
COUNTY: ROCK

STATE R/W PROJECT NUMBER 5990-00-35

CONSTRUCTION PROJECT NUMBER 5990-00-36

PLAT SHEET 4.07

PS&E SHEET _____



TLE COURSE TABLE		
COURSE	BEARING	DISTANCE
140 - 141	N 06°09'59" W	275.73'
141 - 142	N 03°42'14" E	100.50'
142 - 143	N 02°00'24" W	253.78'
143 - 144	S 89°21'52" E	23.02'
144 - 134	S 02°00'24" E	1444.90'
168 - 167	N 89°36'56" E	27.01'
167 - 166	S 60°47'42" W	16.86'
166 - 165	S 02°00'24" E	47.50'
165 - 164	S 65°26'30" E	27.95'
164 - 163	S 02°00'24" E	30.00'
163 - 162	S 21°57'20" W	49.24'
162 - 161	S 58°19'00" E	18.03'
161 - 160	S 02°00'24" E	50.00'
160 - 159	S 09°18'11" W	25.50'
159 - 158	S 02°00'24"E	55.00'
158 - 157	S 21°11'30" W	38.08'
157 - 156	S 02°00'24" E	100.00'
156 - 155	S 20°26'30" E	15.81'
155 - 154	S 02°00'24" E	85.00'

ENCROACHMENT TABLE			
ENCROACHMENT NUMBER	OWNER	TYPE	LOCATION
E-9	F&C AUSTIN LAND CORP	WIRE FENCE	STA. 25+75 - 31+03, 39' LT

REVISION DATE	2/18/2016(NC)		

DATE 2/1/16

GRID FACTOR _____

SCALE, FEET 0 25 50

HWY: AUSTIN ROAD | STATE R/W PROJECT NUMBER 5990-00-35 | PLAT SHEET 4.08

COUNTY: ROCK | CONSTRUCTION PROJECT NUMBER 5990-00-36 | PS&E SHEET 59 E

FILE NAME : 32006-RW PLAT.DWG
LAYOUT NAME - ****

PLOT DATE : 2/18/2016 11:12 AM PLOT BY : CARYN MELLOM PLOT NAME : PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 75

WISCONSIN POWER & LIGHT
OVERHEAD ELECTRIC

STATION 5+90 LT
REMOVE 1-15" CMCP

CLEARING AND GRUBBING
STA. 6+00 - 10+50

REMOVE EXISTING FENCE
STA. 6+00 - 13+30

HOUSE # 5
CHARLES E. DAVIS &
SHARON L. JONES

HOWARD L. & ELIZABETH
F. AUSTIN

HOUSE #215
HOWARD L. AUSTIN

N

TLE

SLOPE INTERCEPT

10+50 MATCH LINE

CHARTER FIBER OPTIC

WETLAND DITCH

AUSTIN ROAD

5+00 N2° 00' 24"W 6+00 7+00 8+00 9+00 10+00

WEST COURT STREET

STATION 5+48
REMOVE 1-24" CMCP

STATION 6+71 RT
REMOVE 1-15" CMCP

5+61 47' RT
IE(W)=835.00

AT&T TELEPHONE

CITY OF JANESVILLE

MULTI-USE PATH

SLOPE INTERCEPT

BEGIN PROJECT NO. 5990-00-36
STA. 5+10
X=480,250.63
Y=267,757.16

5+61 72' RT
IE(E)=834.84

STATION 5+61
1-32' CPRC CL-III, 12" DIA. @ 0.50%
2-RCAEW'S
SKEW 0°

TLE

CITY OF JANESVILLE

TLE

BENCHMARKS				
NO.	STATION	DESCRIPTION	OFFSET	ELEV.
1	5+00	SEC DISK	40' LT	837.77

VCL = 160.00
K = 66.67

STA 5+10.00
EL 837.88

STA 5+11.51
EL 837.86

VPC STA 5+26.67
VPC EL 837.79

LOW POINT STA 5+60.00
LOW POINT EL 837.71

VPI STA 6+06.67
VPI EL 837.39

VPT STA 6+86.67
VPT EL 838.91

STA 8+00.00
EL 841.06

EXISTING GROUND

1.10%

1.90%

-0.50%

-1.08%

FINISHED PROFILE ON R/L

EXISTING ELEVATION ON R/L

FINISHED ELEVATION ON R/L

860 855 850 845 840 835 830

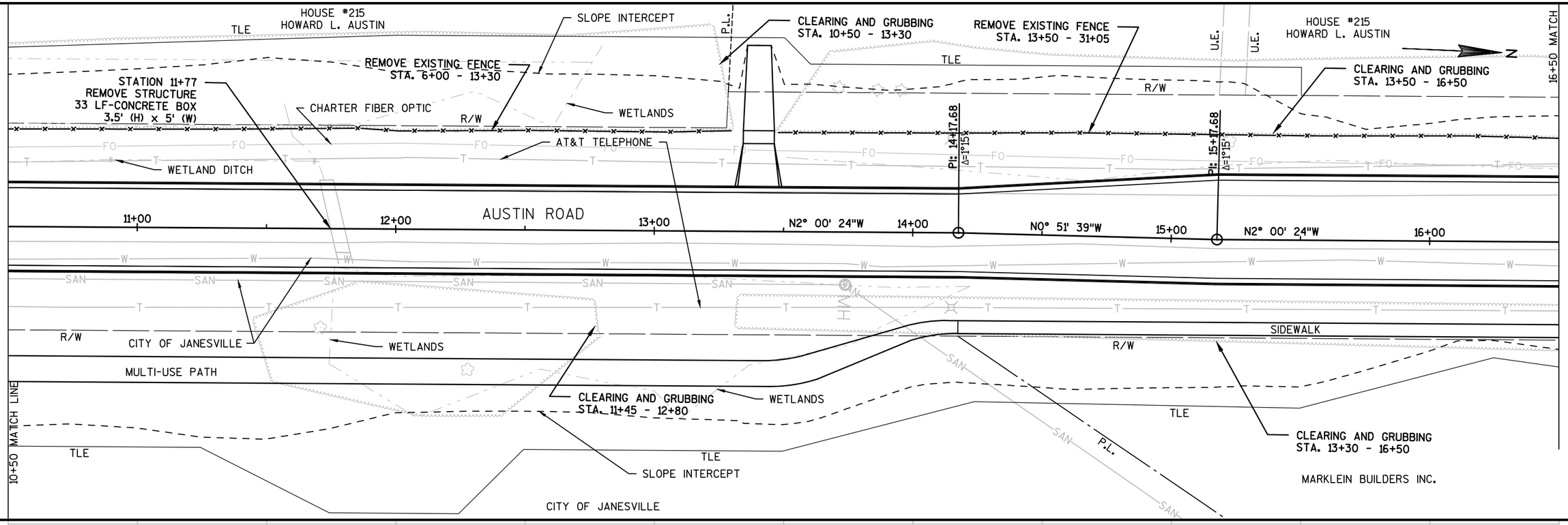
4+50	5+00	+50	6+00	+50	7+00	+50	8+00	+50	9+00	+50	10+00	10+50
837.13	837.99	837.59 **837.71**	837.98 **837.83**	838.64 **838.31**	839.61 **839.16**	840.65 **840.11**	841.37 **841.06**	842.01 **841.61**	842.53 **842.17**	842.94 **842.72**	843.39 **843.27**	843.89 **843.82**

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN AND PROFILE: AUSTIN ROAD	SHEET 60 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_050101_PP_AUSTIN.DWG
LAYOUT NAME - AUSTIN - 1

PLOT DATE : 8/1/2016 11:34 AM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN AND PROFILE: AUSTIN ROAD	SHEET 61 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_050101_PP_AUSTIN.DWG
LAYOUT NAME - AUSTIN - 2
PLOT DATE : 8/1/2016 11:34 AM
PLOT BY : RYAN RUDZINSKI
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

HOUSE #215
HOWARD L. AUSTIN

F & C AUSTIN LAND CORP.

REMOVE EXISTING FENCE
STA. 13+50 - 31+05

SLOPE INTERCEPT

22+50 MATCH LINE

CLEARING AND GRUBBING
STA. 16+50 - 22+35

AT&T TELEPHONE

STATION 22+20
REMOVE 1-18" CMCP

CHARTER FIBER OPTIC

REMOVE STORM MANHOLE
STA 22+20, 9' RT

AUSTIN ROAD

17+00 | N2° 00' 24"W | 18+00 | 19+00 | 20+00 | 21+00 | 22+00

CITY OF JANESVILLE

SIDEWALK

CLEARING AND GRUBBING
STA. 16+50 - 18+30

SLOPE INTERCEPT

16+50 MATCH LINE

REMOVE STORM SEWER
PIPE 40 LF OF 12" RCP

WISCONSIN POWER
& LIGHT GAS

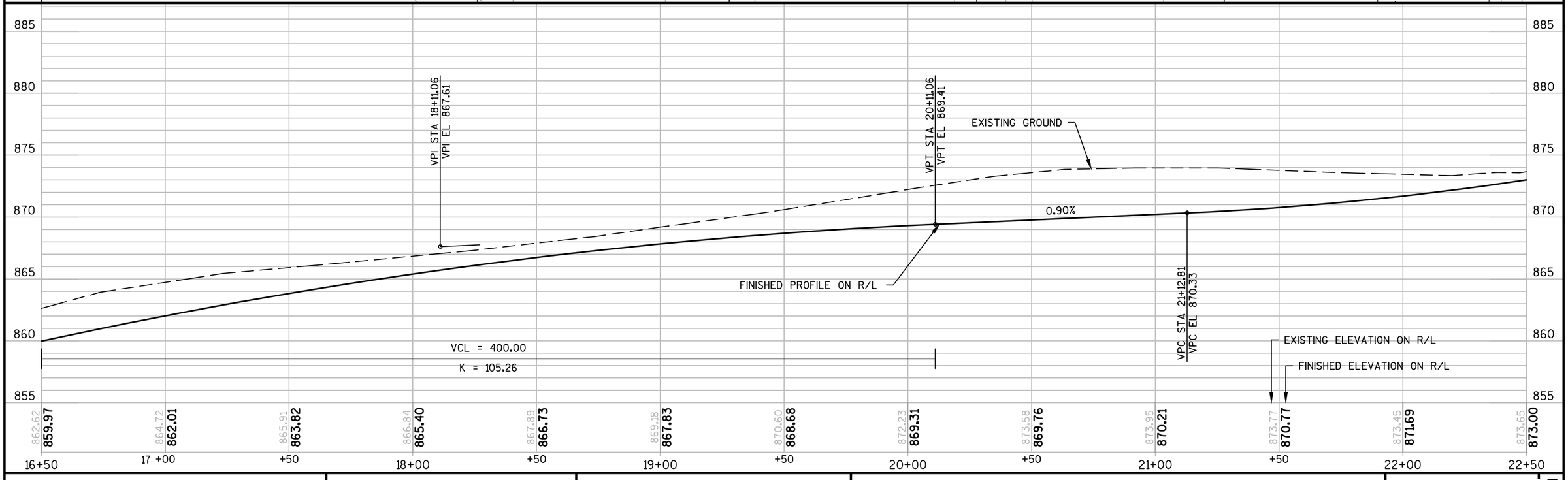
MARKLEIN BUILDERS INC.

HOUSE #226
LINDA M. WEIS

HOUSE #234
TIMOTHY W &
LEIGH E LAMPSON

HOUSE # 244
CYNTHIA A. VISGAR

ARTHUR & NANCY WEIS
JARSTAD



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN AND PROFILE: AUSTIN ROAD	SHEET 62 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_050101_PP_AUSTIN.DWG
LAYOUT NAME - AUSTIN - 3
PLOT DATE : 8/1/2016 11:34 AM
PLOT BY : RYAN RUDZINSKI
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

F & C AUSTIN LAND CORP.

CLEARING AND GRUBBING
STA. 25+10 - 25+80

REMOVE EXISTING FENCE
STA. 13+50 - 31+05

CLEARING AND GRUBBING
STA. 26+45 - 28+50

N

28+50 MATCH LINE

SLOPE INTERCEPT

TLE

AT&T TELEPHONE

CHARTER FIBER OPTIC

R/W

23+00 N2° 00' 24"W 24+00 25+00 26+00 AUSTIN ROAD 27+00 28+00

90°0'0"

CAUTION

CITY OF JANESVILLE

SIDEWALK

KNOLL VIEW DRIVE

22+50 MATCH LINE

N87° 59' 36"E

R/W

WISCONSIN POWER & LIGHT ELECTRIC

TLE

SLOPE INTERCEPT

MICHAEL & CHARLENE WITTLIEFF

P.L.

HOUSE #320
KURT D. MARLING

WISCONSIN POWER & LIGHT GAS

HOUSE #332
KURT D. MARLING

U.E.

HOUSE #346
RIDGEWAY REALTY LLC

HOUSE #406
JAMES D. JR. & BRENDA L. NOLTE

HOUSE #414
MICHAEL E. & MARY JEAN MILLER

5

5

900 895 890 885 880 875 870

VCL = 300.00
K = 65.21

VPI STA 22+62.81
VPI EL 871.68

VPT STA 24+12.81
VPT EL 879.93
VPC STA 24+22.60
VPC EL 880.47

5.50%

EXISTING GROUND

VPI STA 26+07.60
VPI EL 890.64

FINISHED PROFILE ON R/L

1.10%

VPT STA 27+92.60
VPT EL 892.68

VCL = 370.00
K = 84.10

EXISTING ELEVATION ON R/L

FINISHED ELEVATION ON R/L

873.65	874.97	876.78	878.95	881.79	885.01	887.80	889.82	890.95	892.92	894.85	895.00	894.50
873.00	**874.70**	**876.77**	**879.23**	**881.93**	**884.37**	**886.51**	**888.35**	**889.90**	**891.15**	**892.10**	**892.76**	**893.55**
22+50	23+00	+50	24+00	+50	25+00	+50	26+00	+50	27+00	+50	28+00	28+50

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN AND PROFILE: AUSTIN ROAD	SHEET 63 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_050101_PP_AUSTIN.DWG
LAYOUT NAME - AUSTIN - 4
PLOT DATE : 8/1/2016 11:34 AM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

F & C AUSTIN LAND CORP.

CLEARING AND GRUBBING
STA. 28+50 - 31+25

REMOVE EXISTING FENCE
STA. 13+50 - 31+05

TLE

SLOPE INTERCEPT

CHARTER FIBER OPTIC

AT&T TELEPHONE

R/W

R/W

CITY OF JANESVILLE

WISCONSIN POWER & LIGHT
OVERHEAD ELECTRIC

N

AUSTIN ROAD

29+00 30+00 N2° 00' 24"W 31+00 32+00

AUSTIN ROAD

MINERAL POINT AVENUE

END PROJECT NO. 5990-00-36
STA. 31+50
X=480,160.21
Y=270,395.59

CAUTION

SIDEWALK

R/W

AT&T TELEPHONE

CAUTION

TLE

CITY OF JANESVILLE

WISCONSIN POWER &
LIGHT GAS

28+50 MATCH LINE

P.L.

P.L.

P.L.

TLE

TLE

SLOPE INTERCEPT

HOUSE #416
WILLIAM A. &
SUZANNE C.
RENTMEESTER

HOUSE #428
NATHANIEL W. & LISA M. SIMMONS

HOUSE #436
CHRISTINE R. FISHER

U.E.

R/W

VCL = 140.00
K = 68.33

VPC STA 29+32.60
VPC EL 895.65

VPI STA 30+22.60
VPI EL 898.49

VPT STA 31+12.60
VPT EL 899.61

STA 31+50.00
EL 900.08

VPI STA 28+62.60
VPI EL 893.45

EXISTING GROUND

3.15%

1.25%

FINISHED PROFILE ON R/L

VCL = 180.00
K = 94.72

EXISTING ELEVATION ON R/L

FINISHED ELEVATION ON R/L

920 915 910 905 900 895 890

894.50 **893.55** 895.17 **894.70** 897.02 **896.19** 899.09 **897.54** 900.10 **898.62** 900.09 **899.45** 900.08 **900.08** 900.66 903.81

28+50 29+00 +50 30+00 +50 31+00 +50 32+00 +50 33+00 +50 34+00 34+50

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN AND PROFILE: AUSTIN ROAD	SHEET 64 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_050101_PP_AUSTIN.DWG
LAYOUT NAME - AUSTIN - 5
PLOT DATE : 8/1/2016 11:34 AM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

MICHAEL & CHARLENE WITTFIEFF

CITY OF JANESVILLE

CHARTER FIBER OPTIC

SLOPE INTERCEPT

CITY OF JANESVILLE

WISCONSIN POWER & LIGHT ELECTRIC

AT&T TELEPHONE

AUSTIN ROAD

F & C AUSTIN LAND CORP.

SLOPE INTERCEPT

BEGIN CONSTRUCTION
STA. 100+00

N87° 59' 36"E

STA. 100+32, 22' RT
REMOVE STORM MANHOLE

STA. 100+32-101+50
REMOVE STORM SEWER PIPE
118 LF-12" RCP

KNOLL VIEW DRIVE

AT&T TELEPHONE

ABANDON 12" STORM SEWER PIPE

WISCONSIN POWER & LIGHT GAS

WISCONSIN POWER & LIGHT ELECTRIC

SLOPE INTERCEPT

ARTHUR & NANCY WEIS JARSTAD

END CONSTRUCTION
STA. 101+50

WETLANDS

INSTALL 28 LF SAFETY FENCE

CITY OF JANESVILLE

STA 100+00.00 EL 873.63

STA 100+40.00 EL 872.83

VPC STA 100+46.89 VPC EL 872.62

VPI STA 100+96.89 VPI EL 871.12

VPT STA 101+46.89 VPT EL 868.66

STA 101+50.00 EL 868.51

-2.00%

-3.00%

-1.92%

FINISHED PROFILE ON R/L

EXISTING GROUND

VCL = 100.00

K = 52.14

EXISTING ELEVATION ON R/L

FINISHED ELEVATION ON R/L

874.22	871.87	870.35	868.51	867.01
873.63	**872.53**	**870.76**	**868.51**	866.15
				865.48
				865.07

99+00 +50 100+00 +50 101+00 +50 102+00 +50 103+00 +50 104+00 +50

880 875 870 865

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	PLAN AND PROFILE: KNOLL VIEW DRIVE	SHEET 65 E

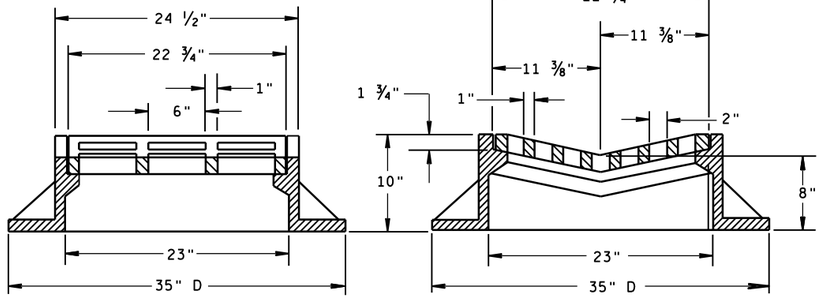
FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_050102_PP_KNOLL VIEW.DWG
LAYOUT NAME - KNOLLVIEW- (1)

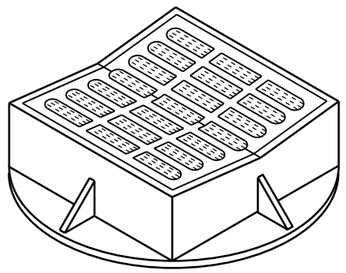
PLOT DATE : 8/1/2016 11:51 AM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

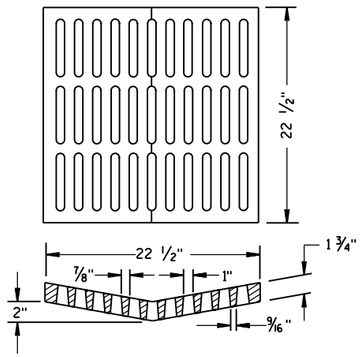
Standard Detail Drawing List

08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08B09-02	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER
08C07-02	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08D01-19	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D04-05	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-18A	CURB RAMPS TYPES 1 AND 1-A
08D05-18B	CURB RAMPS TYPES 2 AND 3
08D05-18C	CURB RAMPS TYPES 4A AND 4A1
08D05-18D	CURB RAMPS TYPE 4B AND 4B1
08D05-18E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E14-01	TRACKING PAD
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09B02-09	CONDUIT
09B04-11	PULL BOX
09C02-07	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09E01-14D	POLE MOUNTINGS FOR LIGHTING UNITS, TYPE 5 (30 FEET)
09E01-14G	HARDWARE DETAILS FOR POLE MOUNTINGS
09E03-05	NON-FREEWAY LIGHTING UNIT POLE WIRING
14A02-01	TREE PLANTING DETAIL
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-03	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C07-13A	PAVEMENT MARKING SYMBOLS
15C07-13B	PAVEMENT MARKING WORDS
15C07-13C	PAVEMENT MARKING ARROWS
15C08-17A	LONGITUDINAL MARKING (MAINLINE)
15C08-17B	PAVEMENT MARKING (TURN LANES)
15C29-03C	URBAN BICYCLE LANE MARKING
15C29-03D	URBAN BICYCLE LANE MARKING
15C29-03E	PAVEMENT MARKING FOR BIKE LANES
15C33-02	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-01A	PAVEMENT MARKING (INTERSECTIONS)



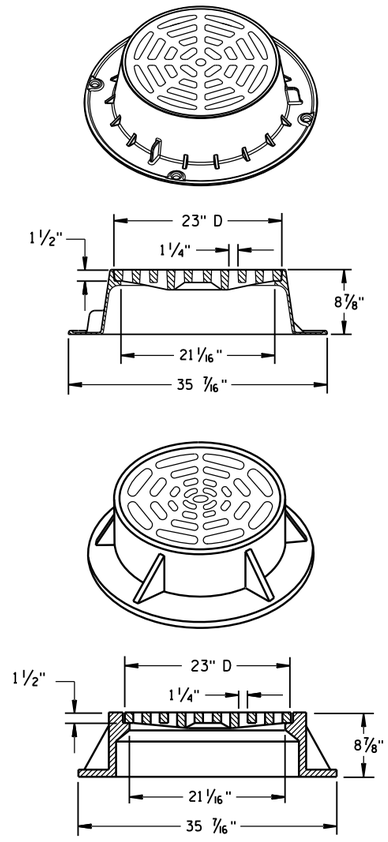


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

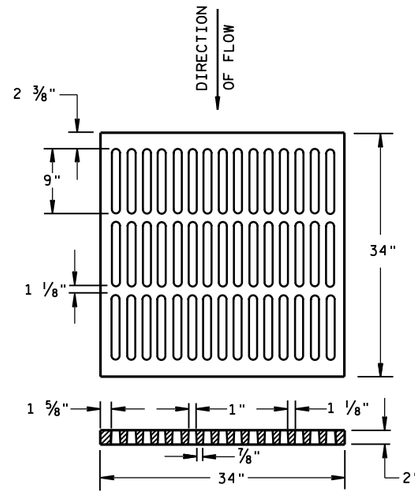
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

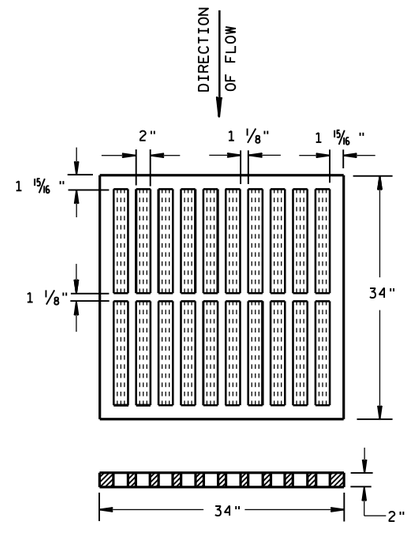
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



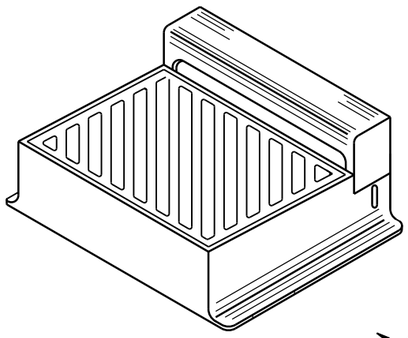
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

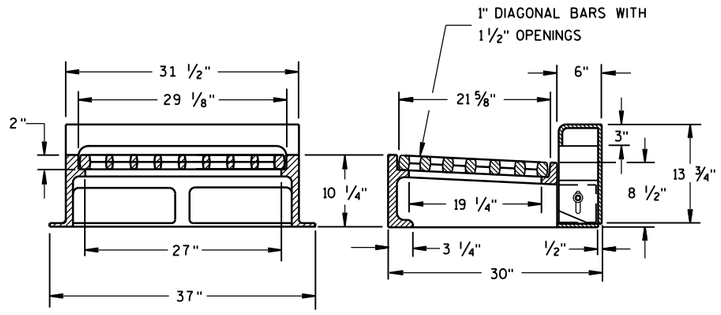


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

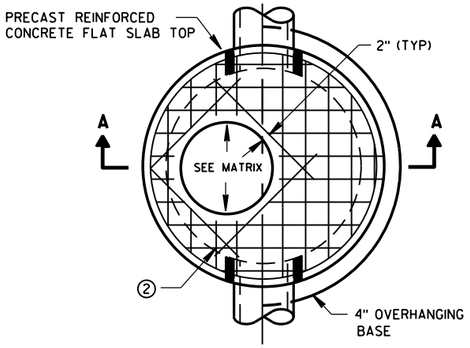
INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

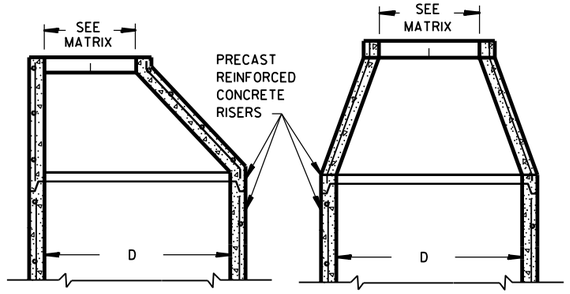
APPROVED
11/27/2013 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

6

S.D.D. 8 A 5-19b

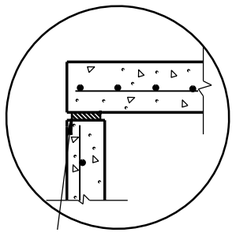


PLAN VIEW CIRCULAR OPENING

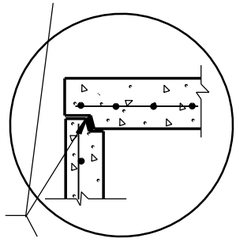


OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP

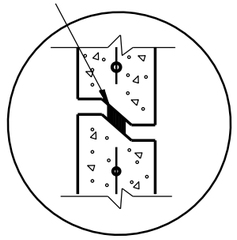


TOP WITH PLAIN END JOINT



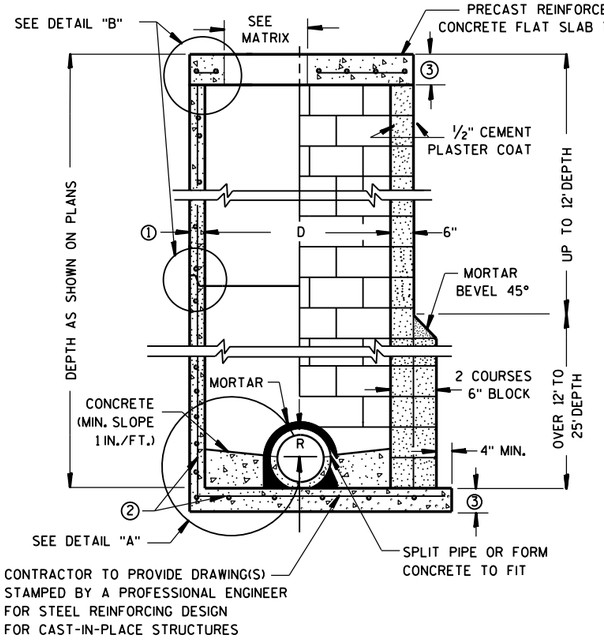
JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C990 (TYP)

TOP WITH TONGUE AND GROOVE JOINT



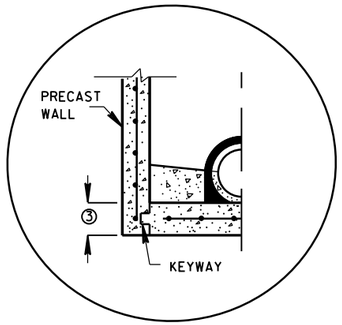
RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

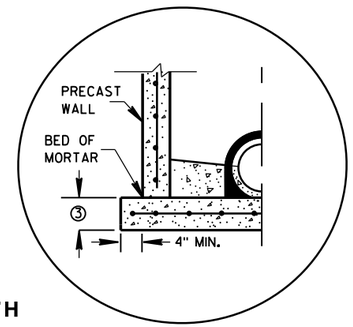


PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

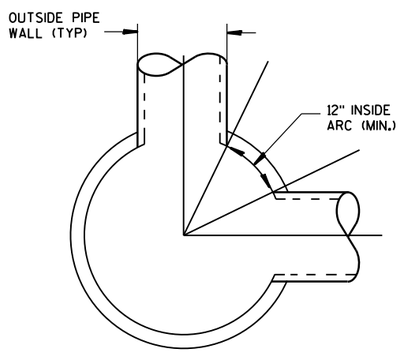


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

DETAIL "A"



DETAIL "C"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF ½" AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

CONCRETE BLOCK WILL NOT BE PERMITED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT, 7 INCHES FOR 6-FT, 8 INCHES FOR 7-FT AND 9 INCHES FOR 8-FT DIAMETER PRECAST MANHOLES.

② FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".

MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE	C	ALL J'S	K	L	M
OPENING SIZE (FT)					
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	36
7-FT	48	36
8-FT	60	42

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

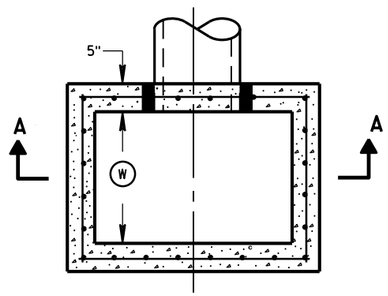
APPROVED

Sept., 2016
DATE

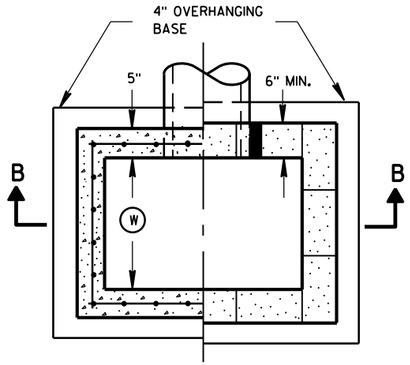
/S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERV

FHWA

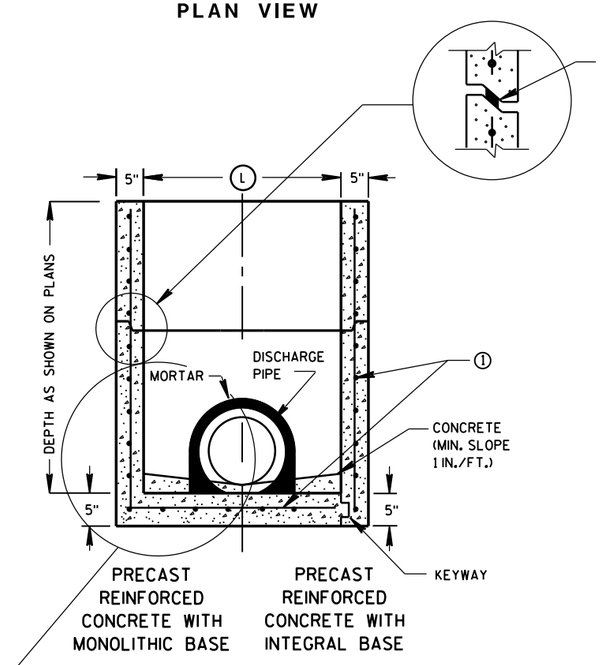
6
S.D.D. 8 B 9-2



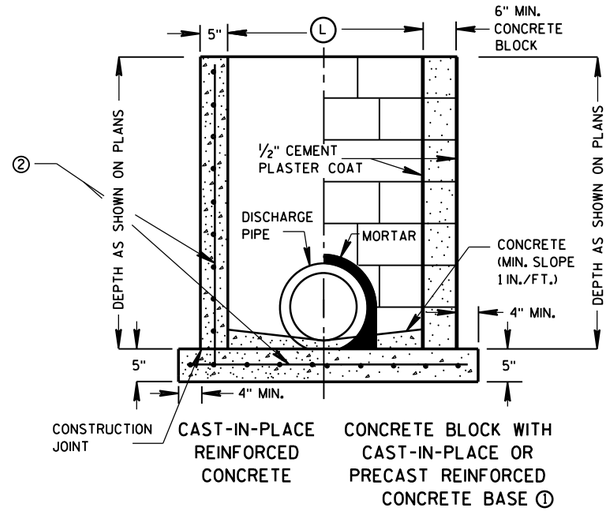
PLAN VIEW



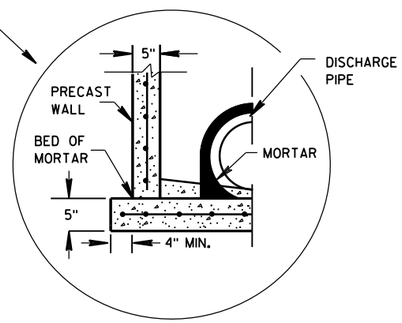
PLAN VIEW



SECTION A-A



SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

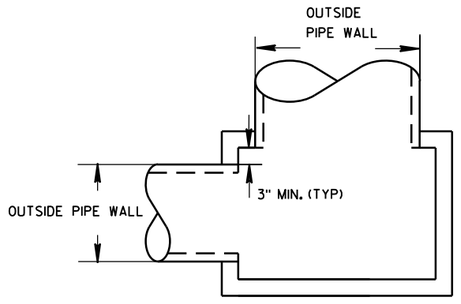
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH (W) (FT)	LENGTH (L) (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



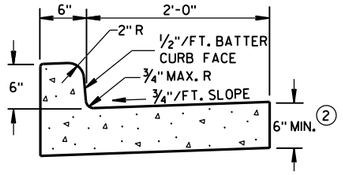
DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

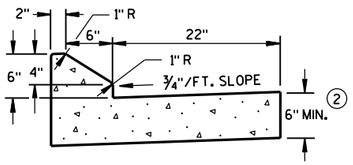
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2016 — DATE
/S/ Rodney Taylor
ROADWAY STANDARDS D IT UNIT SUPERVIS
FHWA

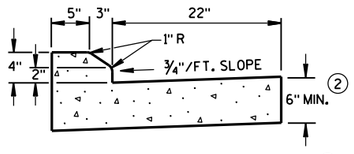
INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT



TYPES A & D ①

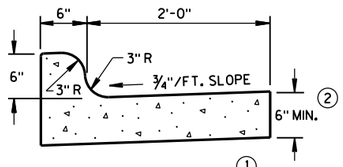


6" SLOPED CURB TYPES G & J ①

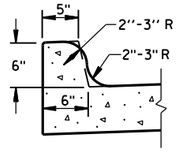


4" SLOPED CURB TYPES G & J ①

CONCRETE CURB & GUTTER 30"

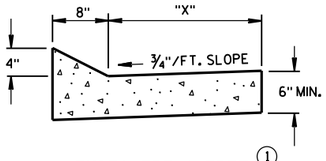


TYPES K & L ①



OPTIONAL CURB SHAPE FOR TYPES K & L ①

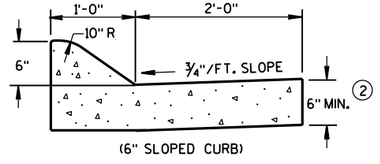
CONCRETE CURB & GUTTER 30"



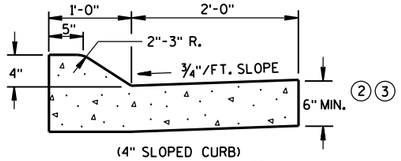
TYPES TBT & TBTT ①

CONCRETE CURB & GUTTER

TBT & TBTT	"X"
30"	22"
36"	28"

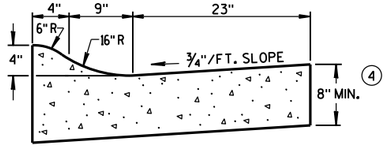


(6" SLOPED CURB)



(4" SLOPED CURB)

TYPES A & D ①



4" SLOPED CURB TYPES R & T ①⑤

CONCRETE CURB & GUTTER 36"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

① TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K, R AND TBTT.

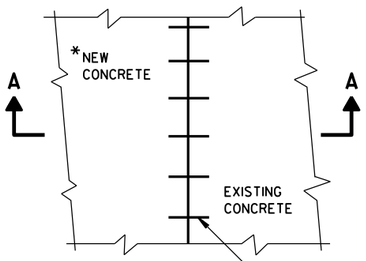
② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

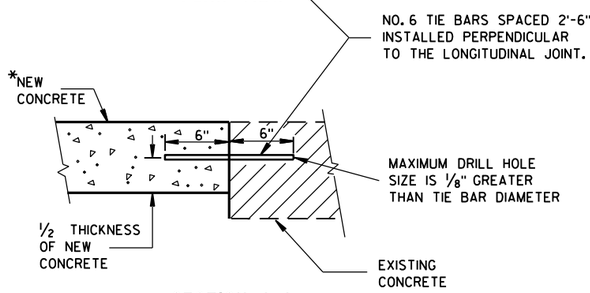
⑤ THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.



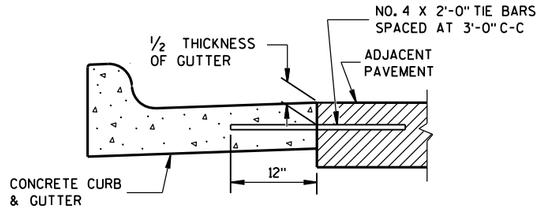
PLAN VIEW

*NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

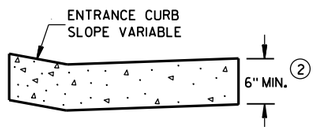


SECTION A-A

TIE BARS DRILLED INTO EXISTING PAVEMENT

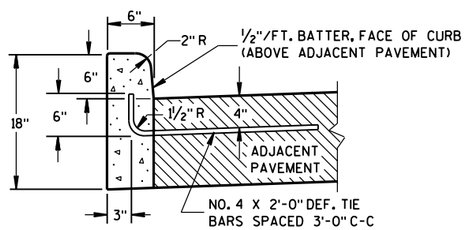


TYPICAL TIE BAR LOCATION ①

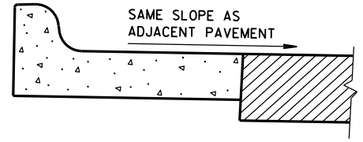


DRIVEWAY ENTRANCE CURB

(WHEN DIRECTED BY THE ENGINEER)

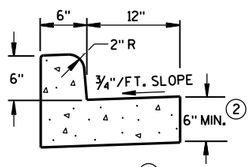


① TYPES A & D



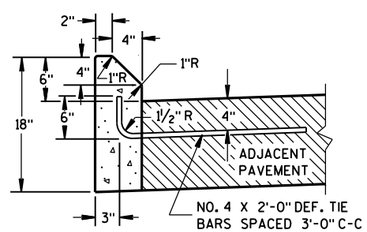
⑥ REVERSE SLOPE GUTTER

(TYPICAL FOR ALL CURB & GUTTER TYPES)



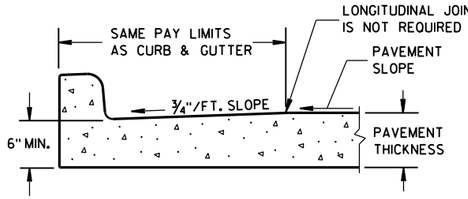
① TYPES A & D

CONCRETE CURB & GUTTER 18"

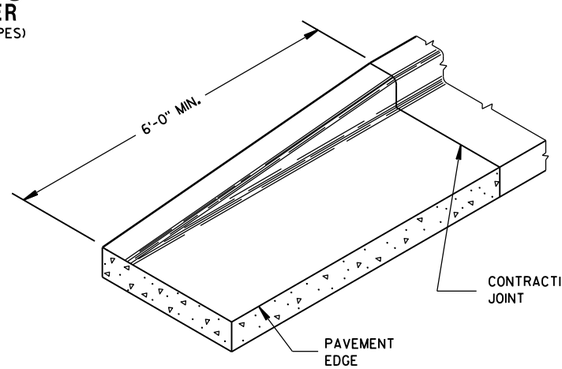


① TYPES G & J

CONCRETE CURB



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER



END SECTION CURB & GUTTER

CONCRETE CURB, CONCRETE CURB & GUTTER AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June, 2016 — DATE

/S/ Jerry H. Zogg — ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

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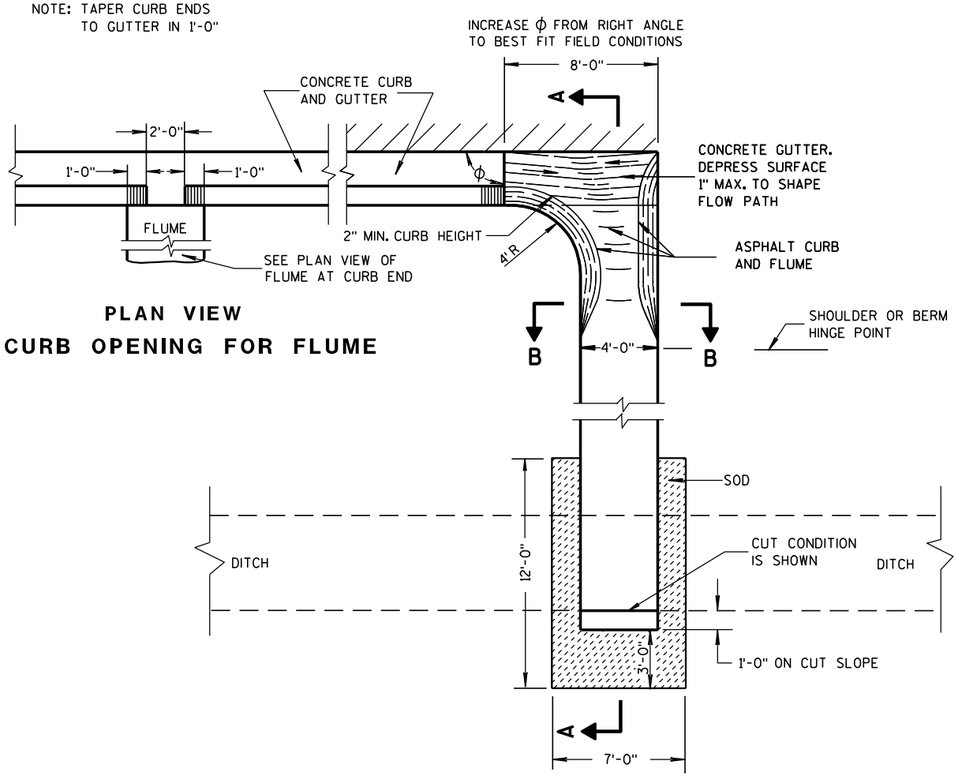
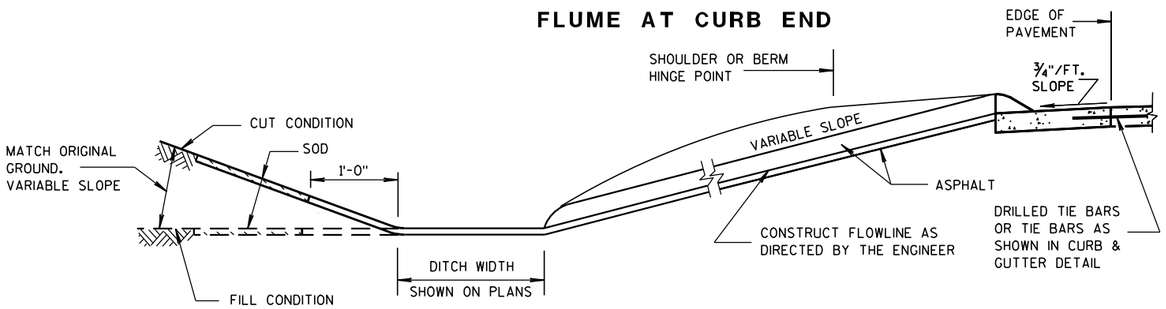
ASPHALTIC FLUME

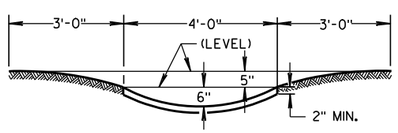


PLAN VIEW
CURB OPENING FOR FLUME

PLAN VIEW
FLUME AT CURB END



SECTION A-A



SECTION B-B

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

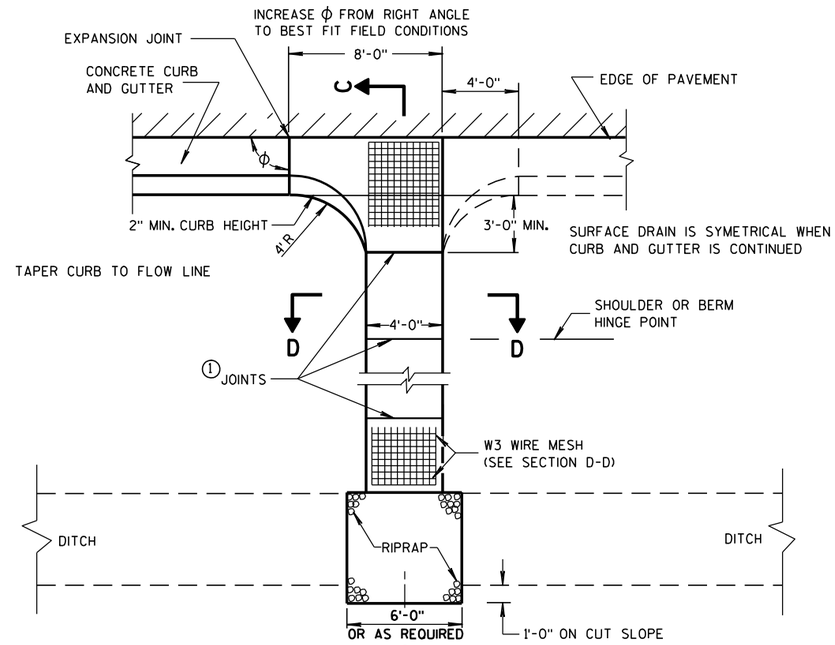
WELDED STEEL WIRE FABRIC SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

① JOINTS SHALL BE $\frac{1}{8}$ TO $\frac{1}{4}$ INCH WIDE BY $1\frac{1}{2}$ INCHES DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.

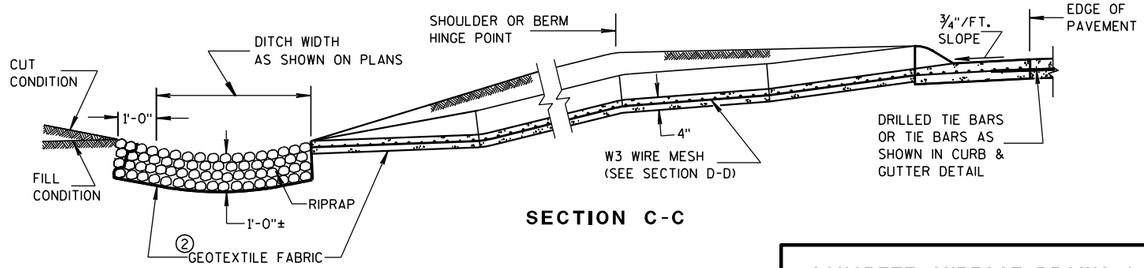
② GEOTEXTILE FABRIC TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.

③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED

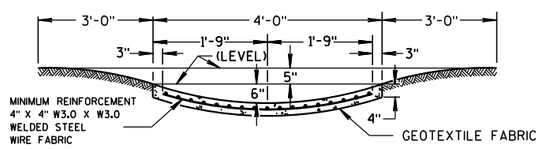
③ CONCRETE SURFACE DRAIN



PLAN VIEW



SECTION C-C



SECTION D-D

CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
9-4-08 DATE
/S/ Jerry H. Zogg ROADWAY STANDARDS ENGINEER
MENT
FHWA

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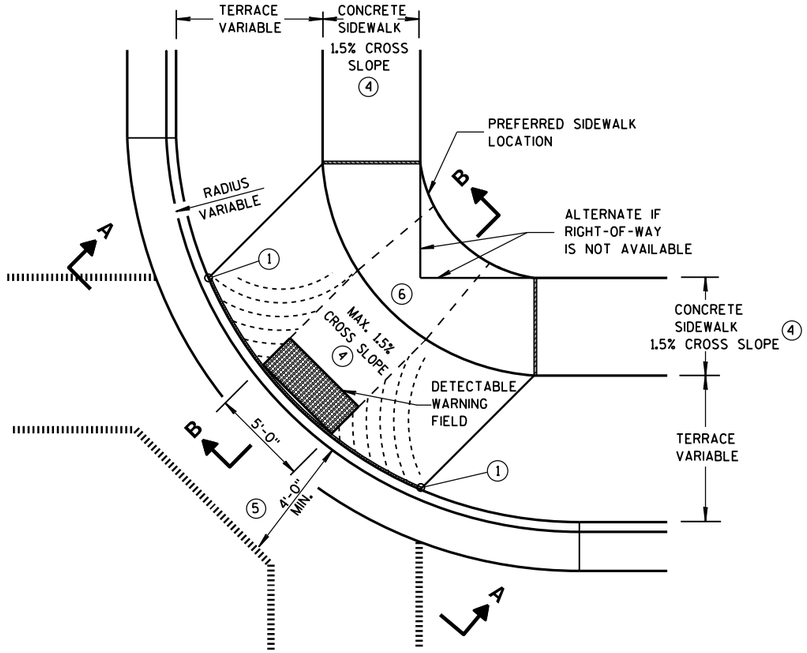
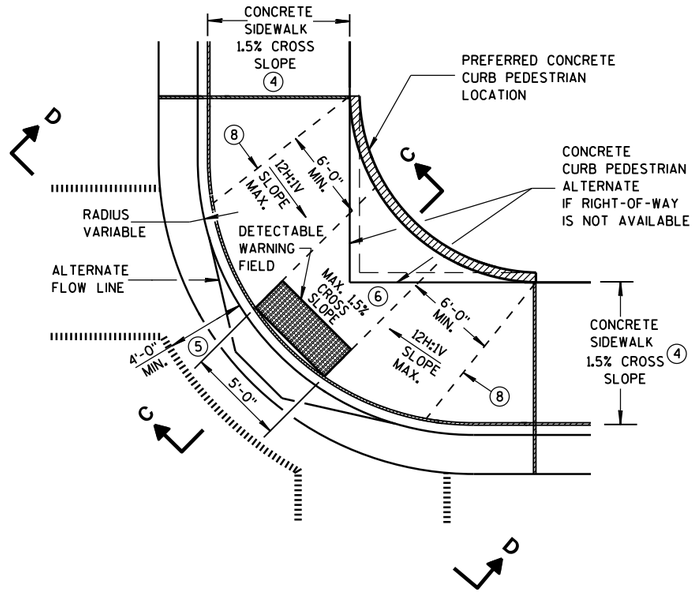
S.D.D. 8 D 4-5



PLAN VIEW

TYPE 1 RAMP

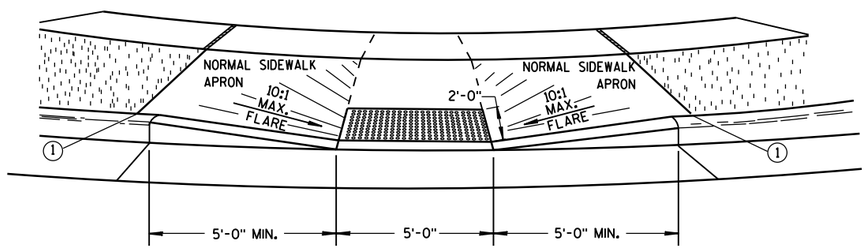
(CENTER OF CORNER RADIUS)



PLAN VIEW

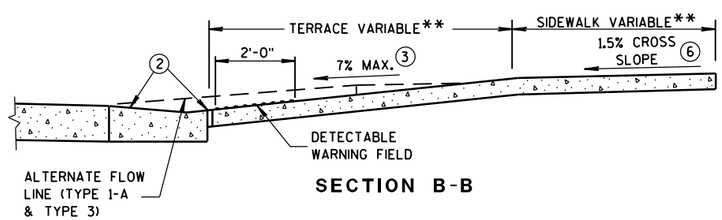
TYPE 1-A RAMP

(NO TERRACE)

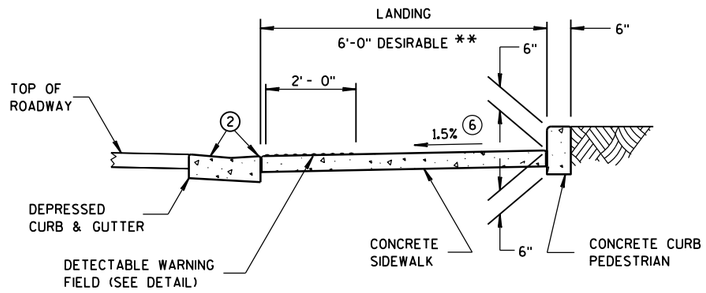


VIEW A-A

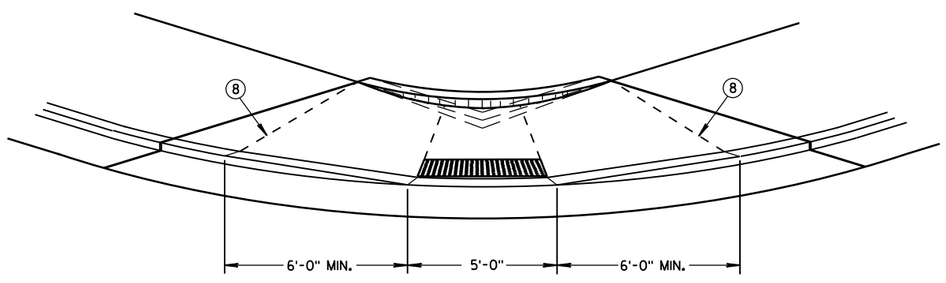
** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B-B



SECTION C-C



VIEW D-D

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

(1) THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL NOT EXCEED 7%.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(5) PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA. (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT

CURB RAMPS TYPES 1 AND 1-A

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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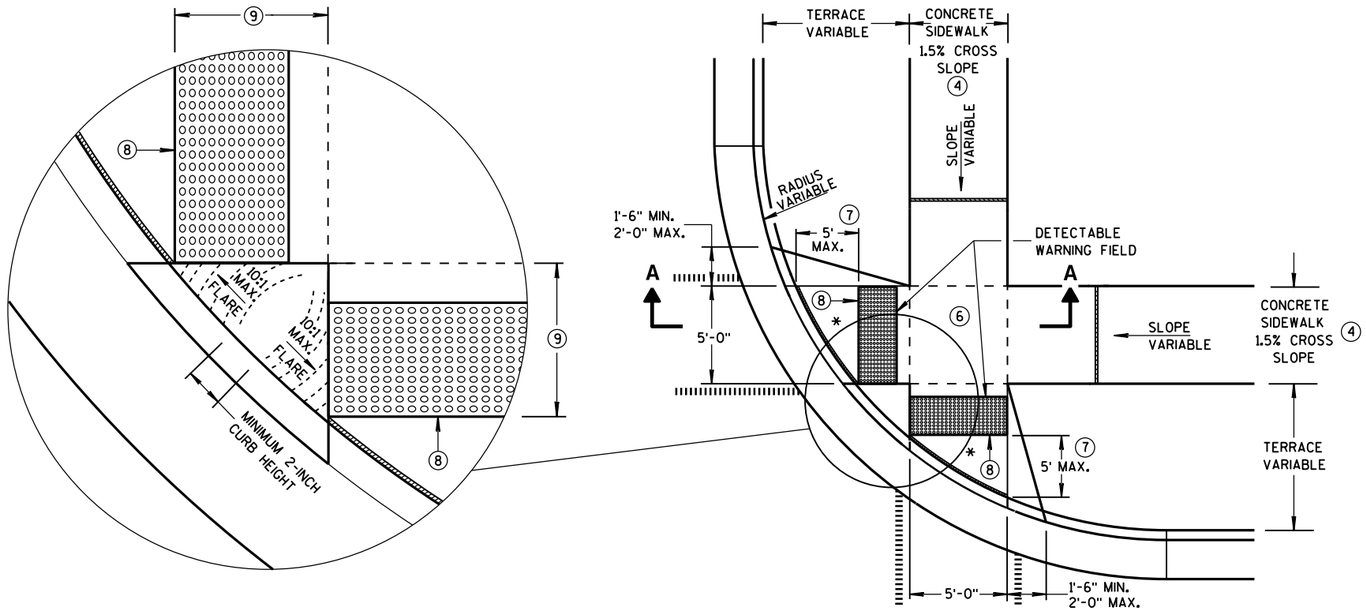
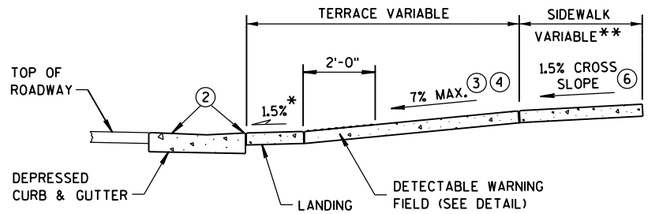


PLAN VIEW

TYPE 2 RAMP

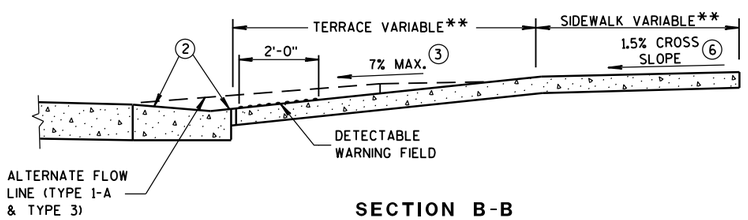
(ON LINE WITH SIDEWALK)

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK



SECTION A-A

** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B-B

GENERAL NOTES

USE THE TYPE 3 RAMP ONLY WHEN A TYPE 1 OR TYPE 2 CANNOT BE ACHIEVED BECAUSE OF FIELD CONDITIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL NOT EXCEED 7%.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

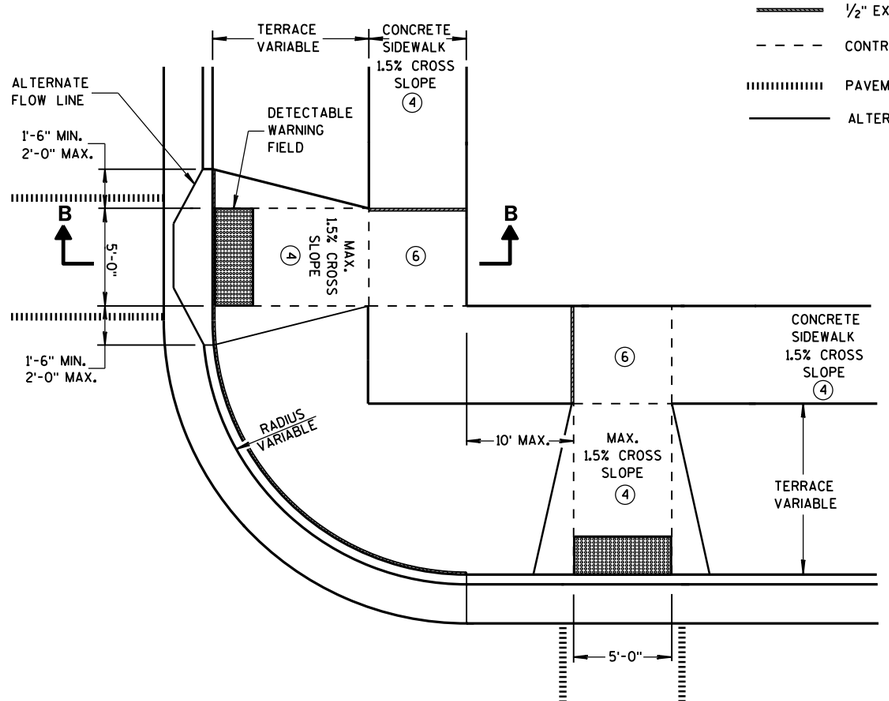
(7) WHEN THIS DISTANCE EXCEEDS 5 FEET, STAGGER ADDITIONAL DETECTABLE WARNING PANEL FORWARD TO REDUCE THIS DISTANCE. PROVIDE MINIMUM 12-INCH ROW OVERLAP TO AVOID SIDESTEP OF DOME DETECTION. USE EQUAL-SIZE PANELS TO DEVELOP OVERLAPPING, STAGGERED ROWS. ALIGN DOMES BETWEEN OVERLAPPING ROWS AND IN DIRECTION OF PEDESTRIAN TRAVEL.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(9) WHEN THIS DISTANCE IS LESS THAN 6'-0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



PLAN VIEW

TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)

CURB RAMPS TYPES 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

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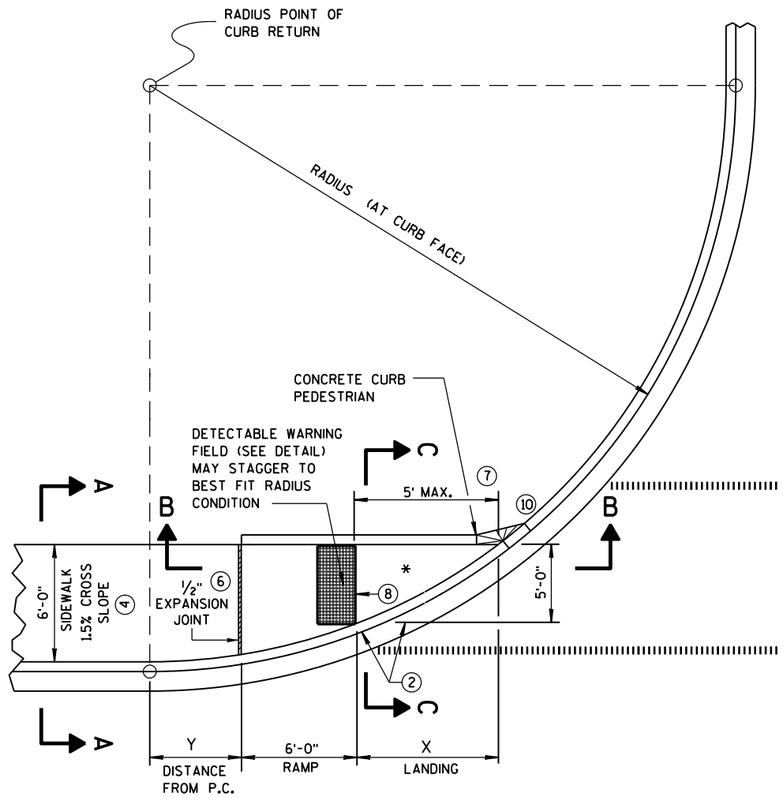
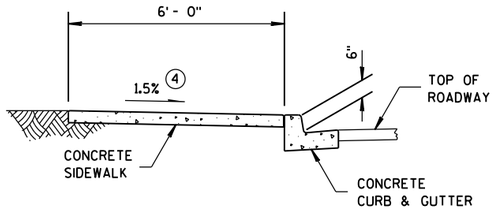


CURB RAMP TYPE 4A
PLAN VIEW

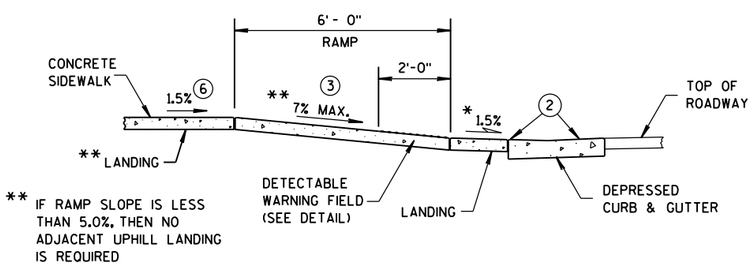
RADIUS (AT CURB FACE)	X	Y
20 FEET	7'-11"	0'-2"
30 FEET	10'-2¾"	1'-7½"
40 FEET	12'-1¼"	2'-10"
50 FEET	13'-8¾"	3'-10¾"
60 FEET	15'-2"	4'-10¼"

INTERMEDIATE RADII CAN BE INTERPOLATED



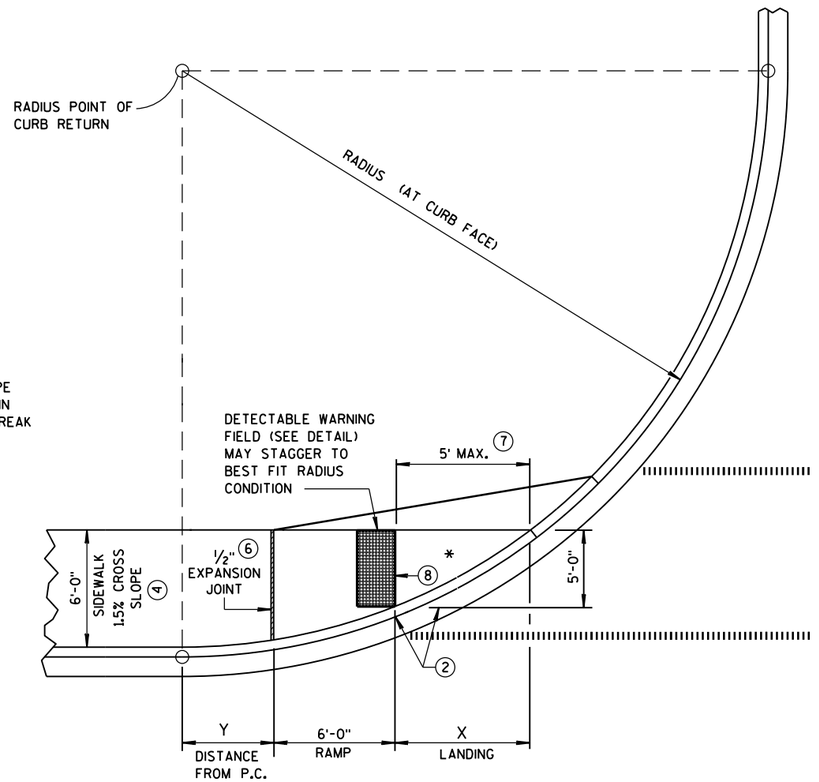
SECTION A-A FOR TYPE 4A

SECTION C-C FOR TYPE 4A



SECTION B-B FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK



CURB RAMP TYPE 4A1
PLAN VIEW

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL NOT EXCEED 7%.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

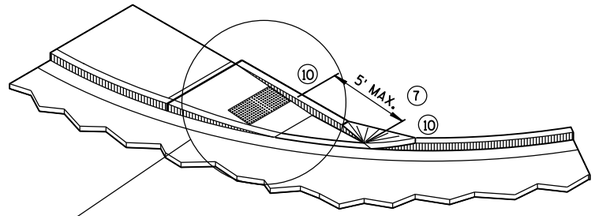
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

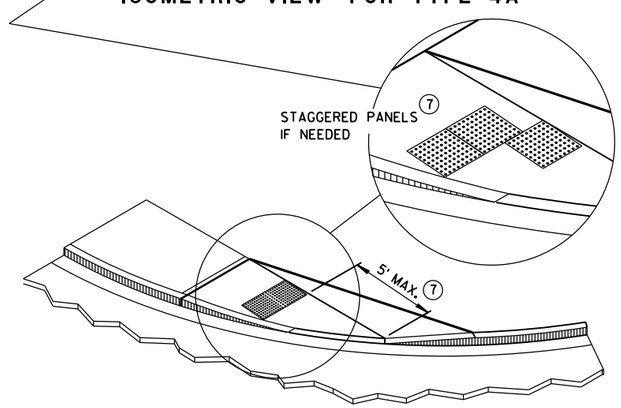
(7) WHEN THIS DISTANCE EXCEEDS 5 FEET, STAGGER ADDITIONAL DETECTABLE WARNING PANEL FORWARD TO REDUCE THIS DISTANCE. PROVIDE MINIMUM 12-INCH ROW OVERLAP TO AVOID SIDESTEP OF DOME DETECTION. USE EQUAL-SIZE PANELS TO DEVELOP OVERLAPPING, STAGGERED ROWS. ALIGN DOMES BETWEEN OVERLAPPING ROWS AND IN DIRECTION OF PEDESTRIAN TRAVEL.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(10) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

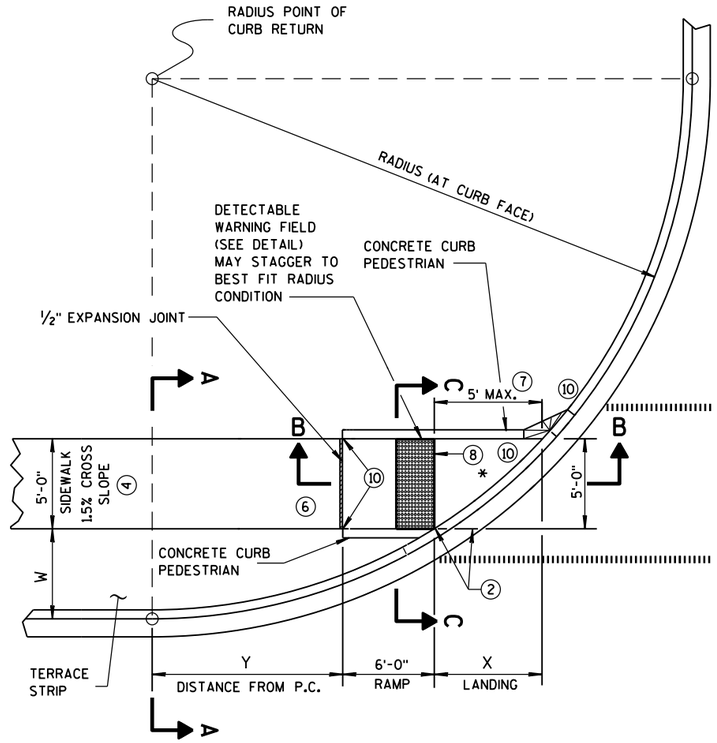
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPES 4A AND 4A1

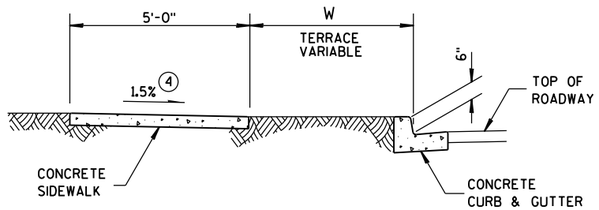
STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

6

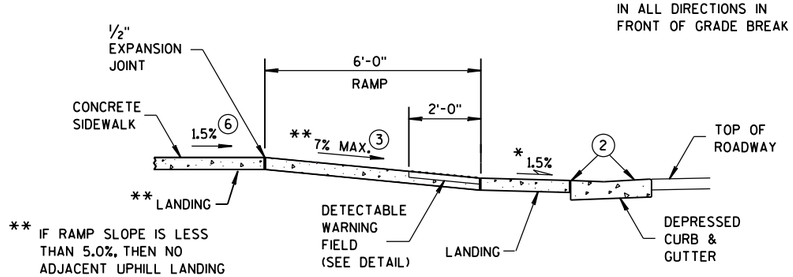
S.D.D. 8 D 5-18c



CURB RAMP TYPE 4B
PLAN VIEW



SECTION A-A FOR TYPE 4B



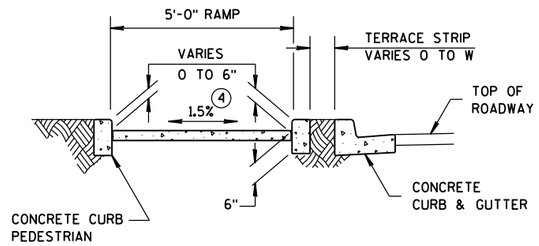
SECTION B-B FOR TYPE 4B

LEGEND

½" EXPANSION JOINT-SIDEWALK

CONTRACTION JOINT FIELD LOCATED

PAVEMENT MARKING CROSSWALK (WHITE)



SECTION C-C FOR TYPE 4B

RADIUS (AT CURB FACE)	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"	
	X	Y	X	Y	X	Y	X	Y	X	Y
20 FEET	5'-9¾"	3'-6½"	4'-11½"	5'-1¾"	4'-3¼"	6'-5½"	3'-8¾"	7'-6¾"	3'-3"	8'-6¼"
30 FEET	7'-9¼"	5'-10½"	6'-9½"	7'-11¼"	6'-0¼"	9'-8"	5'-5"	11'-1¾"	4'-10¾"	12'-5¾"
40 FEET	9'-4"	7'-10"	8'-2¾"	10'-3"	7'-4¾"	12'-3¾"	6'-8½"	14'-1¼"	6'-1¾"	15'-8½"
50 FEET	10'-8"	9'-6½"	9'-5½"	12'-3¼"	8'-6½"	14'-7½"	7'-9¾"	16'-8¼"	7'-2½"	18'-6¼"
60 FEET	11'-10¼"	11'-0¾"	10'-6½"	14'-1¼"	9'-6½"	16'-8½"	8'-9¼"	18'-11¾"	8'-1½"	21'-0½"

INTERMEDIATE RADII CAN BE INTERPOLATED

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL NOT EXCEED 7%.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

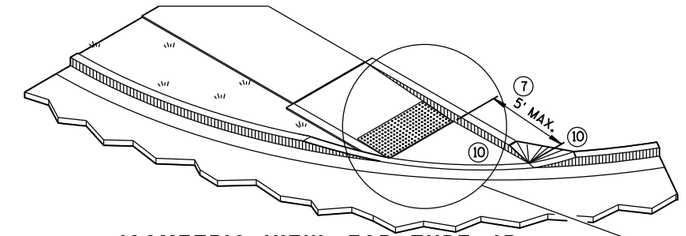
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

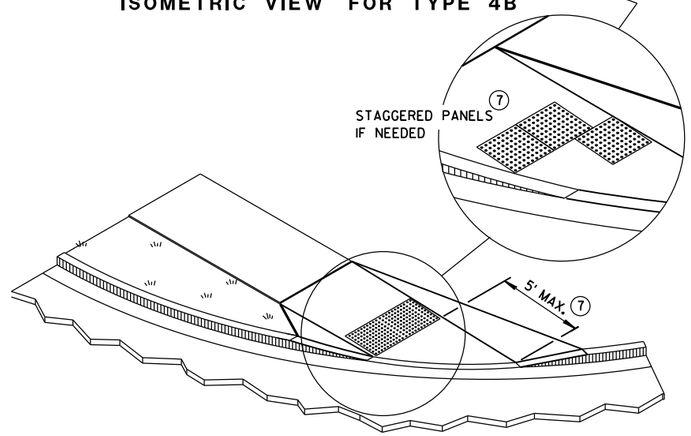
(7) WHEN THIS DISTANCE EXCEEDS 5 FEET, STAGGER ADDITIONAL DETECTABLE WARNING PANEL FORWARD TO REDUCE THIS DISTANCE. PROVIDE MINIMUM 12-INCH ROW OVERLAP TO AVOID SIDESTEP OF DOME DETECTION. USE EQUAL-SIZE PANELS TO DEVELOP OVERLAPPING, STAGGERED ROWS. ALIGN DOMES BETWEEN OVERLAPPING ROWS AND IN DIRECTION OF PEDESTRIAN TRAVEL.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

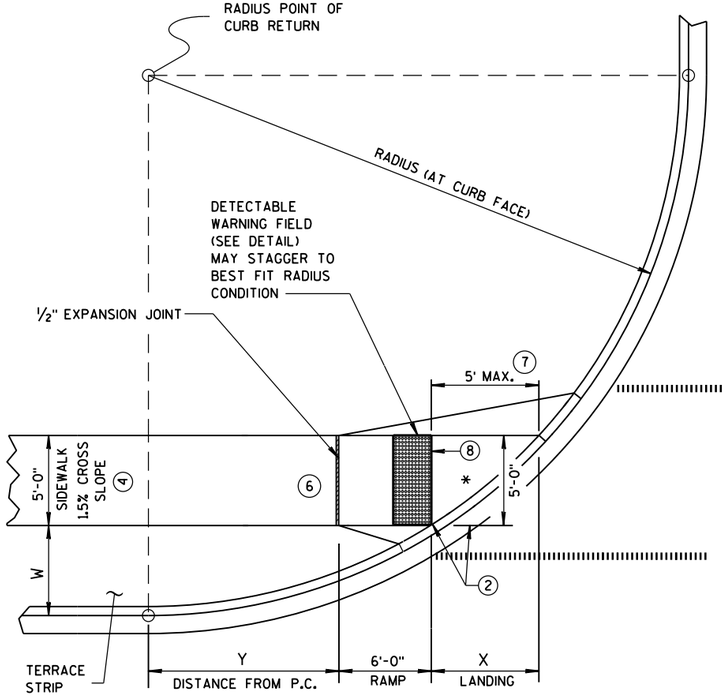
(10) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1



CURB RAMP TYPE 4B1
PLAN VIEW

CURB RAMPS
TYPE 4B AND 4B1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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S.D.D. 8 D 5-18d

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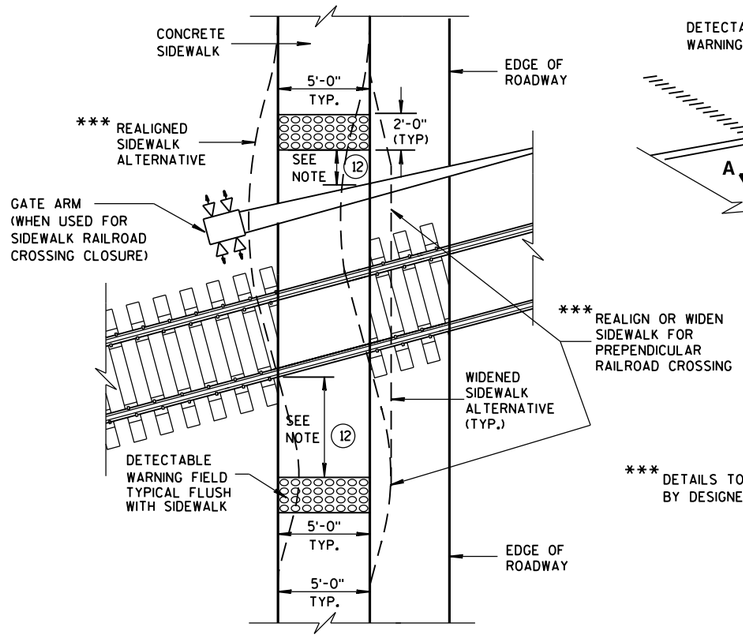
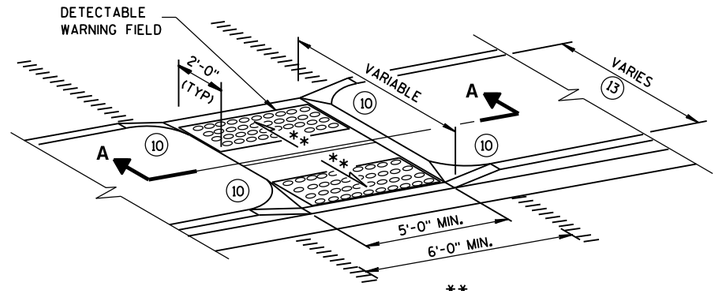
S.D.D. 8 D 5-18d



*** DETAILS TO BE DETERMINED BY DESIGNER

TYPE 8
DETECTABLE WARNINGS AT RAILROAD CROSSING



MEDIAN ISLAND NON-ELEVATED CROSSING
TYPE 5

GENERAL NOTES

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL NOT EXCEED 7%.

(3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

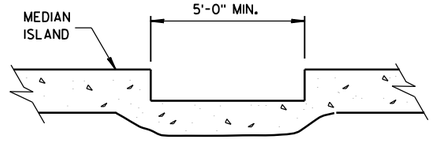
(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET.

(10) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

(11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

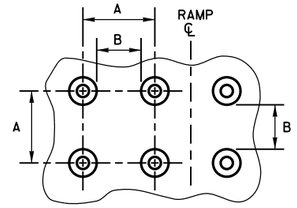
(13) DO NOT INSTALL DETECTABLE WARNING FIELDS IF MEDIAN WIDTH BETWEEN BACK OF CURBS IS LESS THAN 6 FEET.



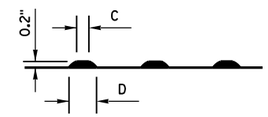
SECTION A-A

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

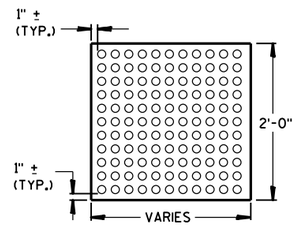


PLAN VIEW

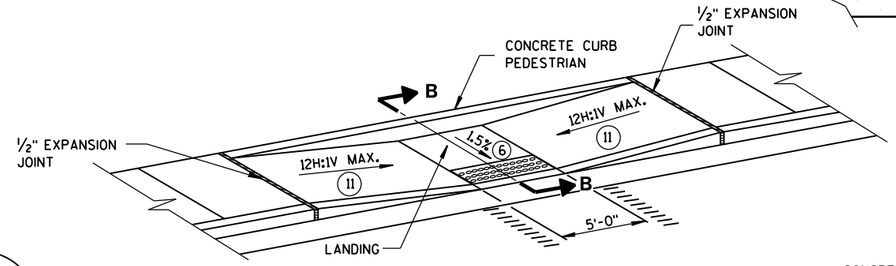


ELEVATION VIEW

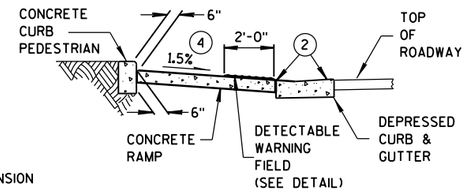
TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL



PLAN VIEW
DETECTABLE WARNING FIELD (TYPICAL)

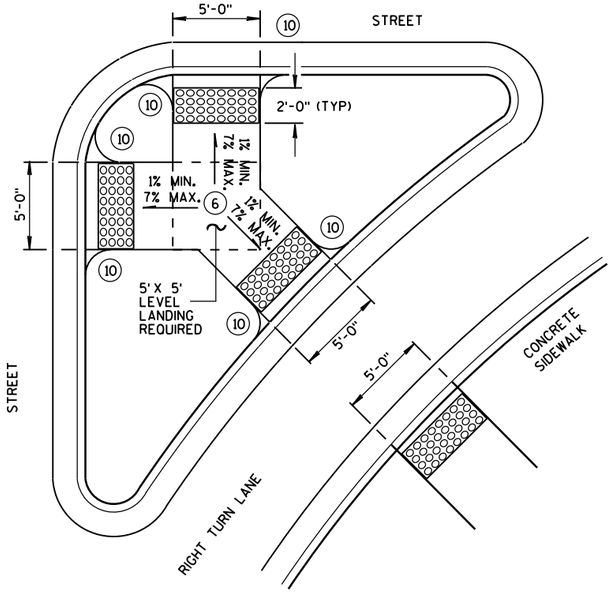


MID-BLOCK CROSSING
TYPE 7A

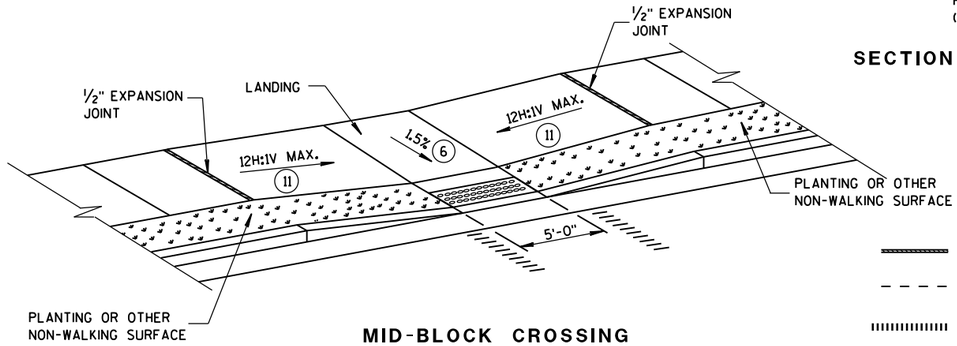


SECTION B-B

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



TYPE 6
DETECTABLE WARNING AT ISLANDS



MID-BLOCK CROSSING
TYPE 7B

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

LEGEND

- 1/2" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPES 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2016
DATE
FHWA

/S/ Rodney Taylor
ROADWAY STANDARDS UNIT SUPERV

ENT

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S.D.D. 8 D 5-18e

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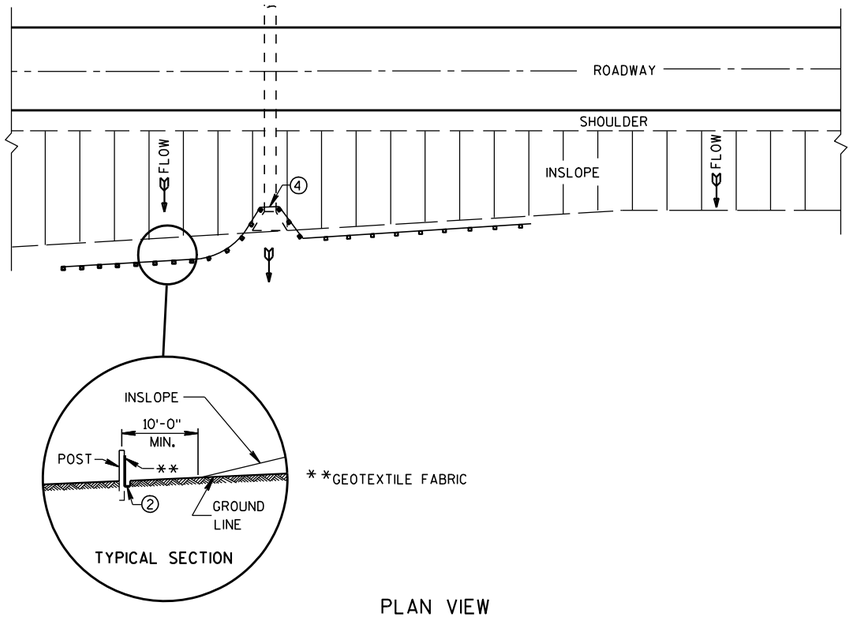
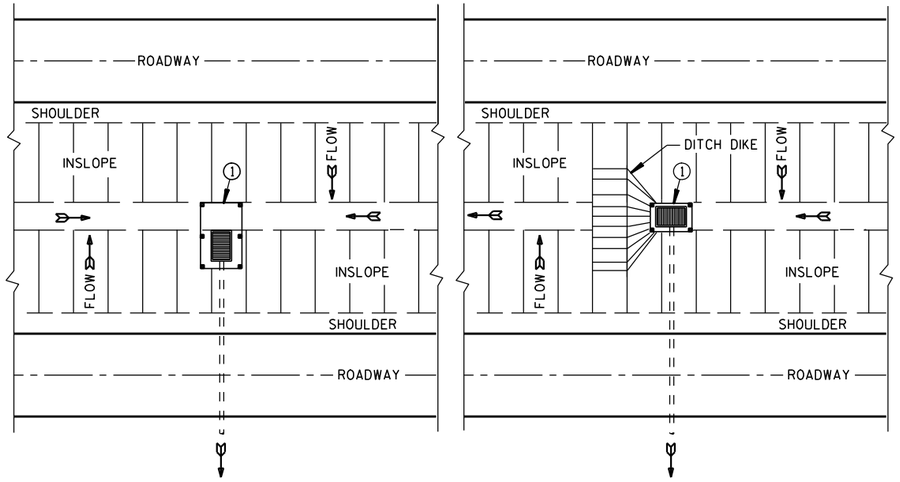
S.D.D. 8 D 5-18e



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

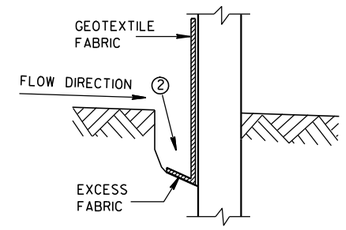
① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.

② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.

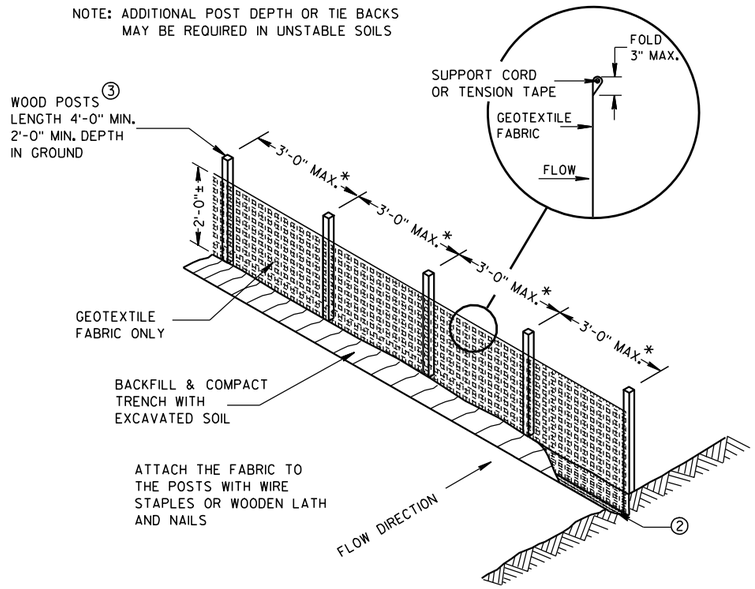
③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.

④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.

⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

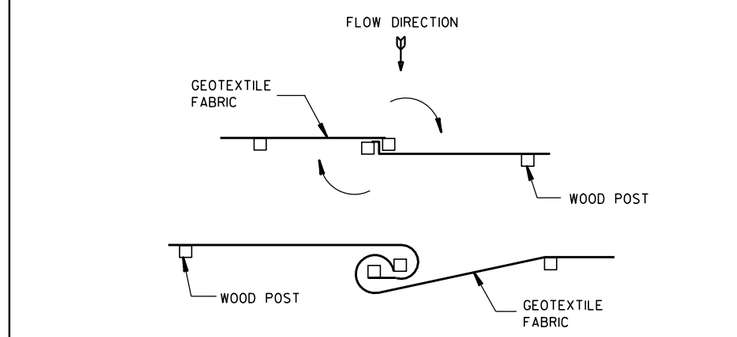


TRENCH DETAIL

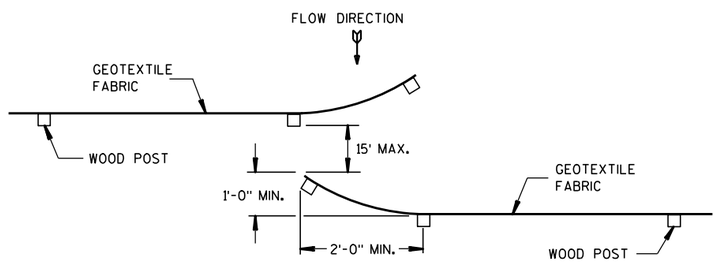


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

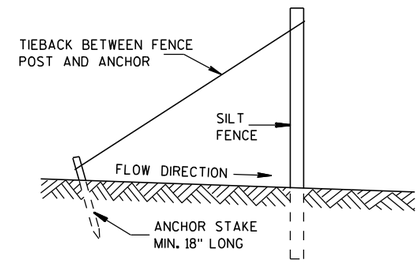


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

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S.D.D. 8 E 9-6

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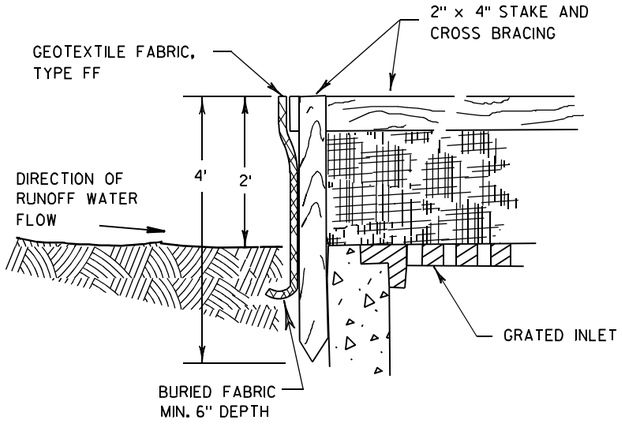
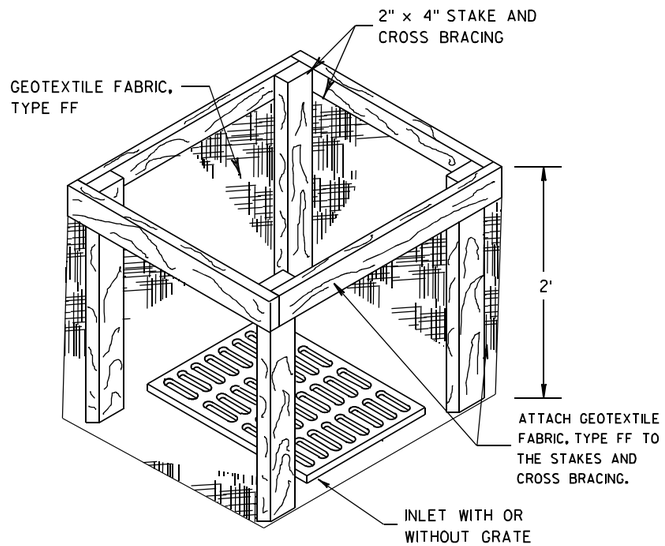
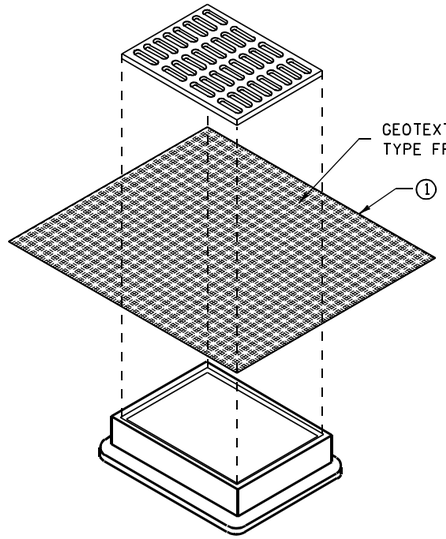
S.D.D. 8 E 9-6



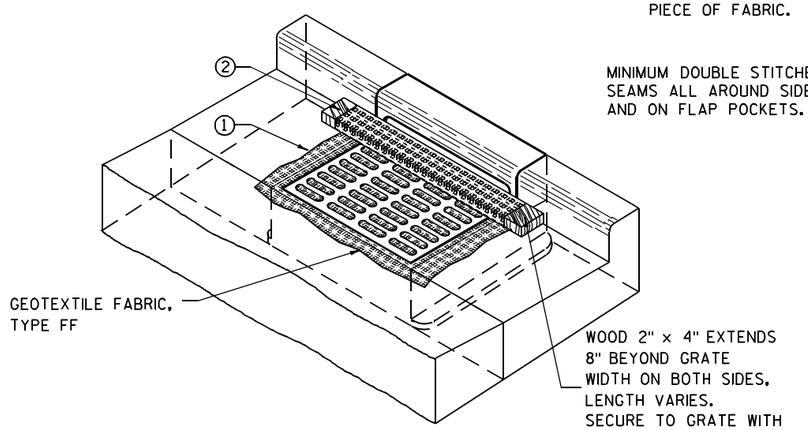


INLET PROTECTION, TYPE A

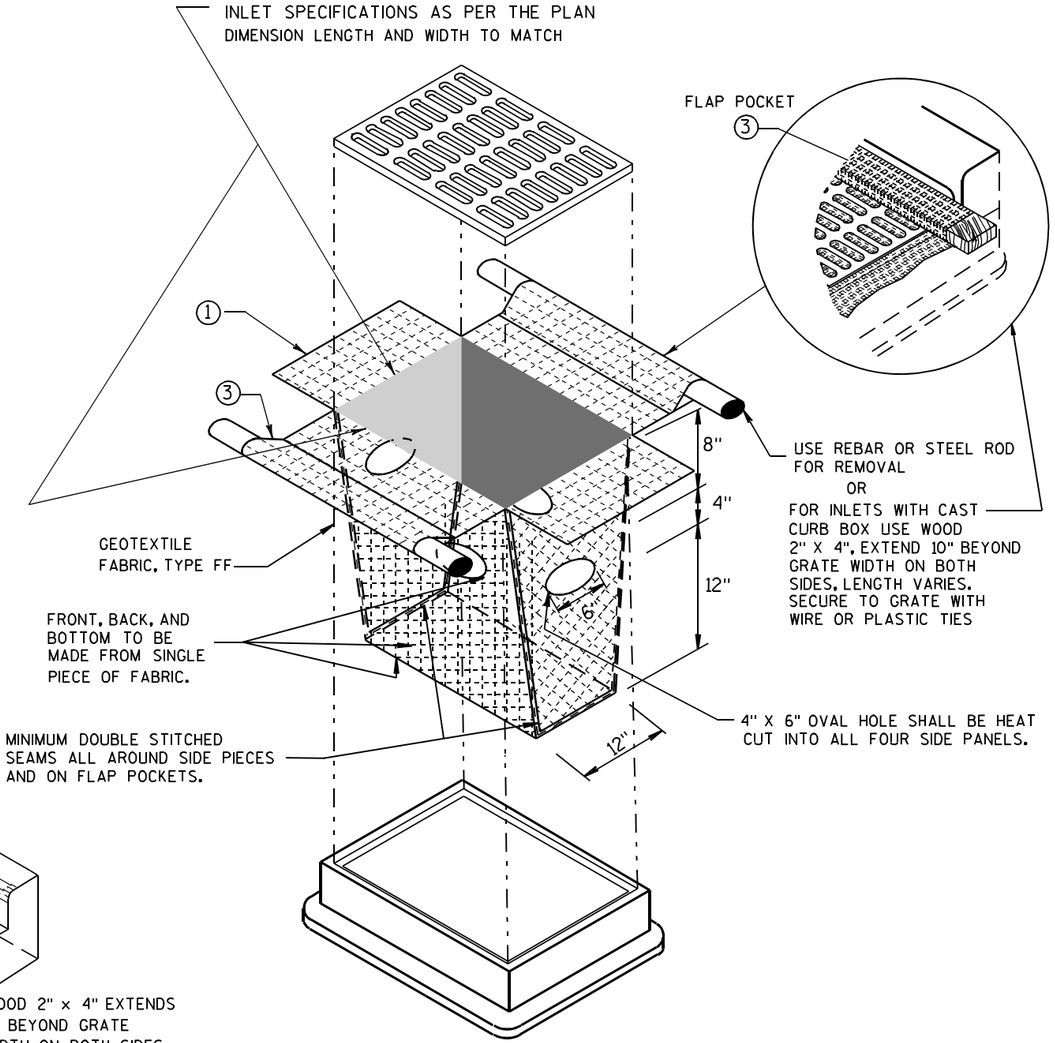


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

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S.D.D. 8 E 10-2

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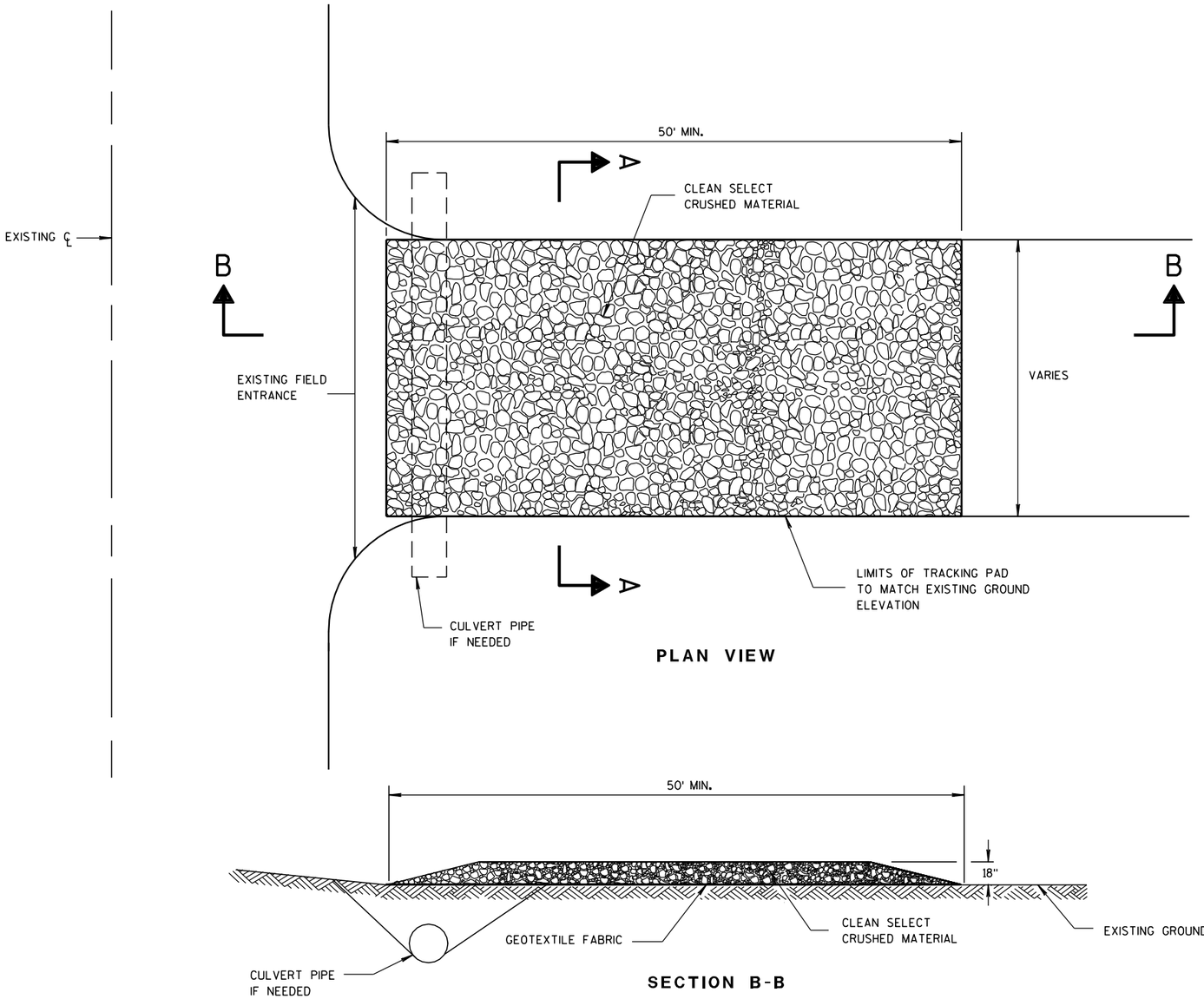
S.D.D. 8 E 10-2



PLAN VIEW

SECTION B-B

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TRACKING PAD SHALL BE INSPECTED DAILY. DEFICIENT AREAS SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

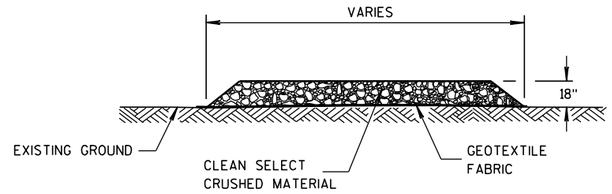
TRACKING PAD TO BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT.

SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY, AROUND OR CONVEYED UNDER THE TRACKING PAD.

CULVERT PIPE OR OTHER BMP USED TO DIVERT WATER AWAY, AROUND OR UNDER THE TRACKING PAD SHALL BE DESIGNED TO CONVEY THE 2 YEAR - 24 HOUR EVENT.

THE COST OF ADDITIONAL BMP TO DIVERT WATER ARE INCIDENTAL TO THE TRACKING PAD BID ITEM.



SECTION A-A

TRACKING PAD

6

S.D.D. 8 E 14-1

TRACKING PAD

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/24/2011	/S/ Jerry H. Zoda	
DATE	ROADWAY STANDARDS ENGINEER	ENT

FHWA

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S.D.D. 8 E 14-1

METAL APRON ENDWALLS

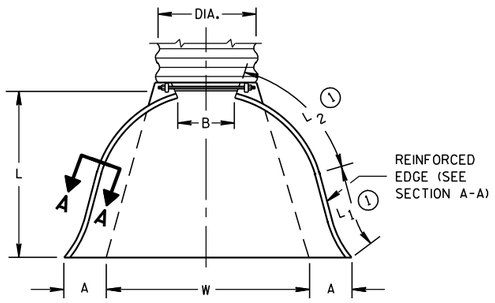
PIPE DIA. (IN.)	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109×	.105×	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109×	.105×	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109×	.105×	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109×	.105×	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109×	.105×	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109×	.105×	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109×	.105×	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

× EXCEPT CENTER PANEL SEE GENERAL NOTES

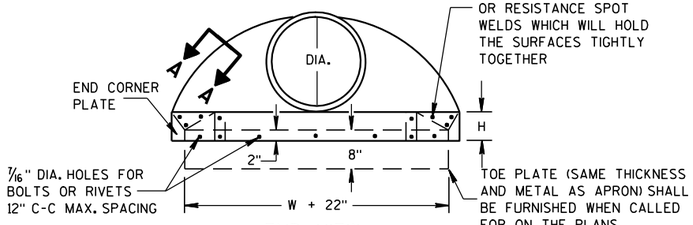
REINFORCED CONCRETE APRON ENDWALLS

PIPE DIA. (IN.)	DIMENSIONS (Inches) T	A	B	C	D	E	G	APPROX. SLOPE
12	2	4	24	48 7/8	72 1/8	24	2	3 to 1
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1
48	5	24	72	26	98	84	5	3 to 1
54	5 1/2	27	65	* 33 1/4 - ** 35	* 98 1/4 - ** 100	90	5 1/2	2 2/5 to 1
60	6	* 30 - ** 35	60	39	99	96	5	2 to 1
66	6 1/2	* 24 - ** 30	* 72 - ** 78	* 21 - ** 27	99	102	5 1/2	2 to 1
72	7	* 24 - ** 36	78	21	99	108	6	2 to 1
78	7 1/2	* 24 - ** 36	78	21	99	114	6 1/2	2 to 1
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1

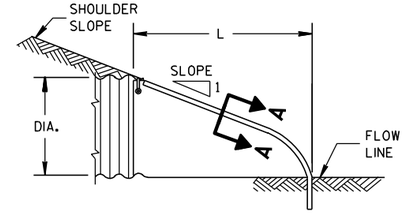
* MINIMUM
** MAXIMUM



PLAN VIEW

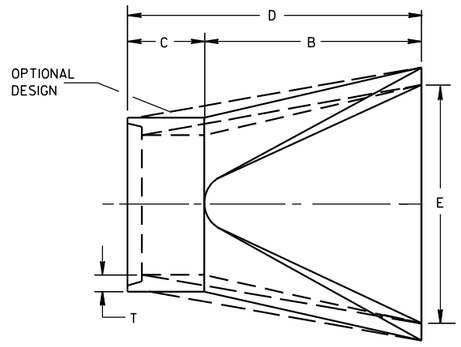


END VIEW

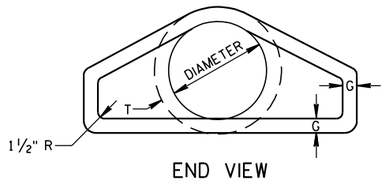


SIDE ELEVATION

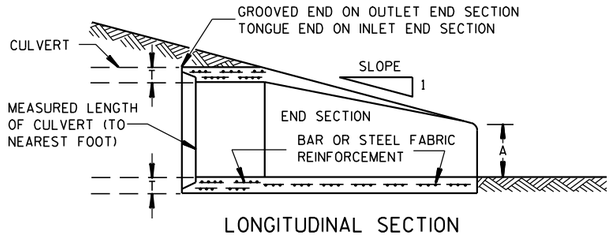
METAL ENDWALLS



PLAN

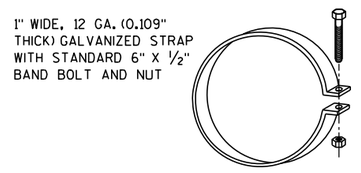


END VIEW



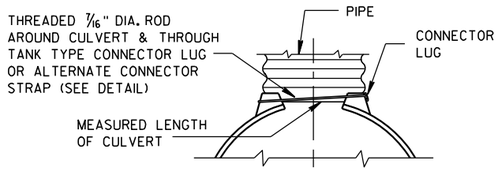
LONGITUDINAL SECTION

CONCRETE ENDWALLS

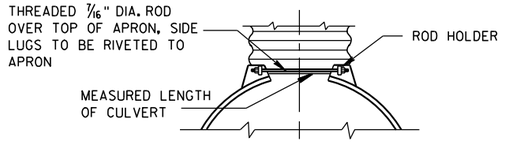


ALTERNATE FOR TYPE 1 CONNECTION

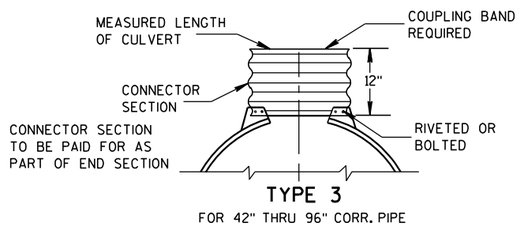
END SECTION CONNECTOR STRAP



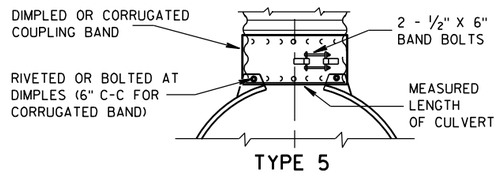
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

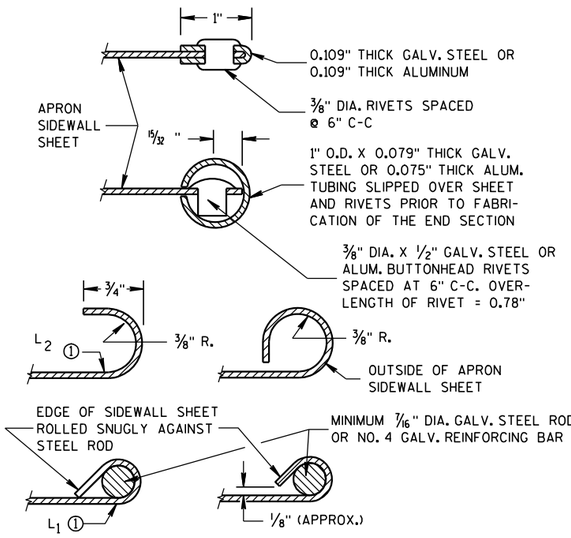
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL. AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

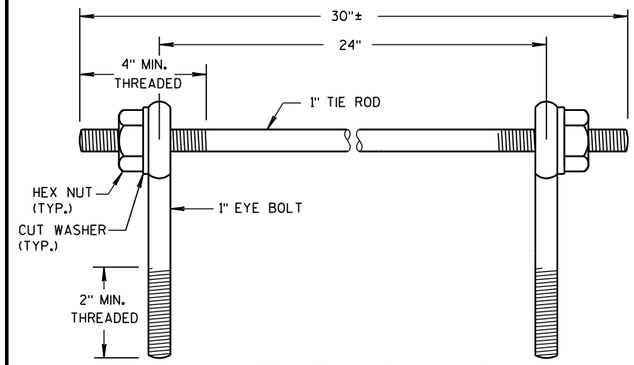
① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR CULVERT PIPE

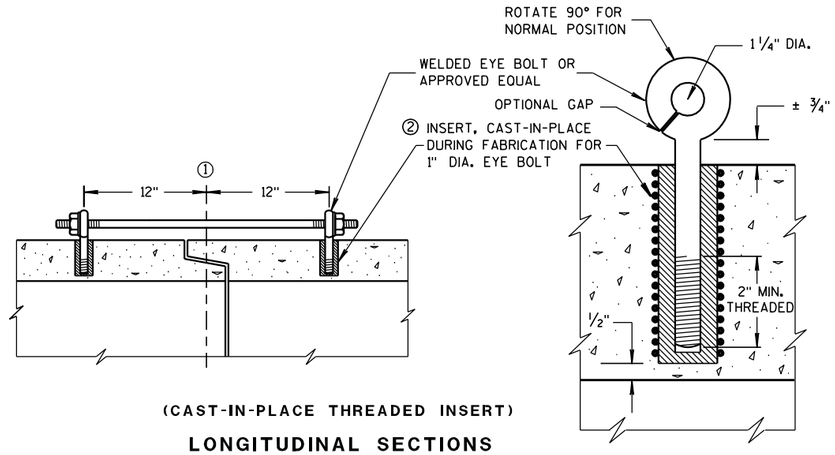
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 DATE
/S/ Rory L. Rhinesmith
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6
S.D.D. 8 F 1-11



EYE BOLTS AND TIE ROD



(CAST-IN-PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

① ℄ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

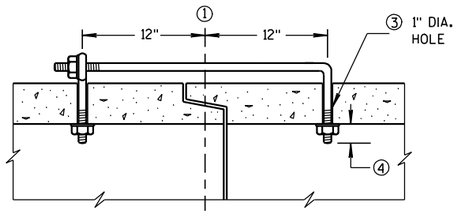
② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ℄ OF TONGUE AND GROOVE.

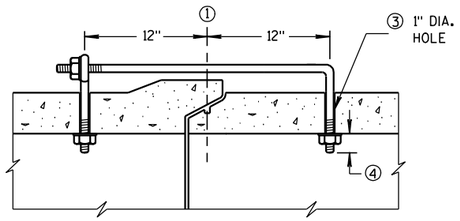
④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.

⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.



(TONGUE & GROOVE PIPE)

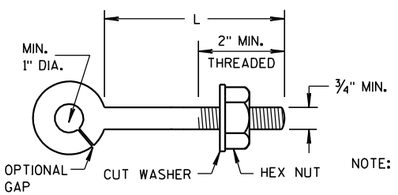


(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

EYE BOLT DIMENSION TABLE

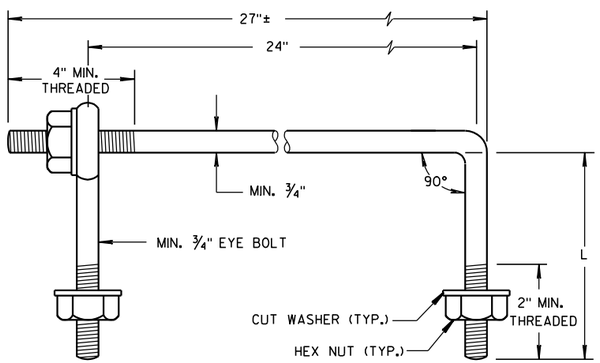
PIPE SIZE	L = LENGTH	
	TONGUE & GROOVE PIPE	MODIFIED BELL PIPE
18" TO 24"	4 1/2"	6 1/4"
30"	5"	7"
36"	5 1/2"	7"
42"	6"	
48"	6 1/2"	
60"	7 1/2"	
66"	8"	



NOTE:

EYE BOLT

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD

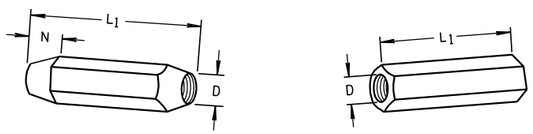
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12-60	5/8	5/8	5	1/2
66-84	3/4	3/4	5	1/2
90-108	1	1	7	1 1/16

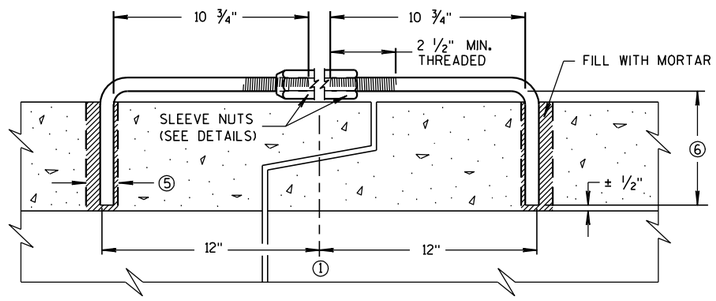
DIMENSIONS SHOWN ARE IN INCHES



TAPERED PLAIN

RIGHT AND LEFT THREADS

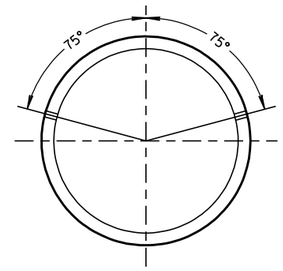
SLEEVE NUTS



LONGITUDINAL SECTION

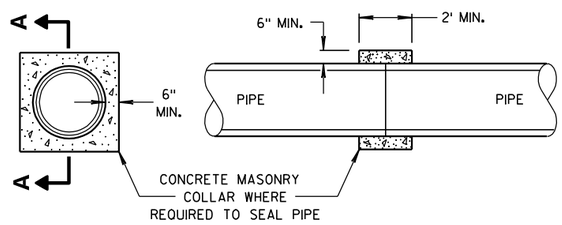
(JOINT TIES FOR 12" TO 108" DIA. CONCRETE PIPE)

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A-A

CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

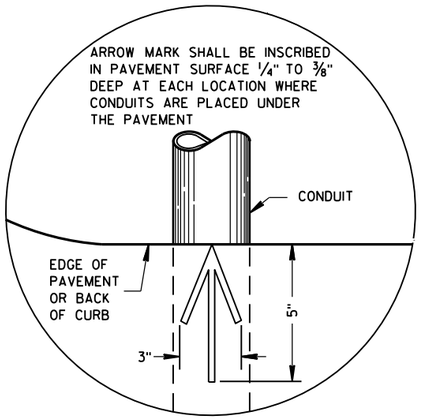
APPROVED
6/5/2012 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

6

S.D.D. 8 F 4-7

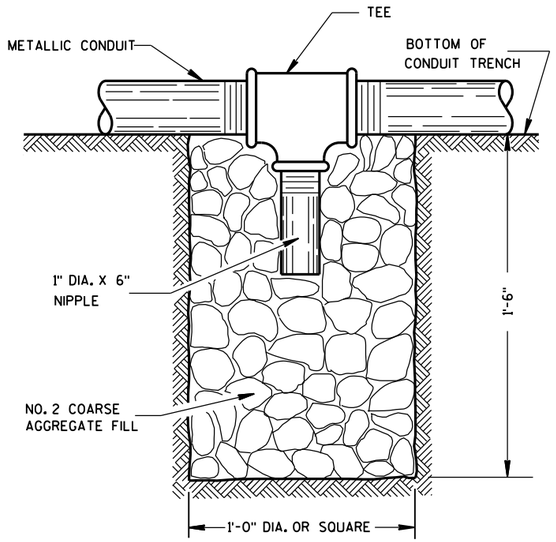
6

S.D.D. 8 F 4-7



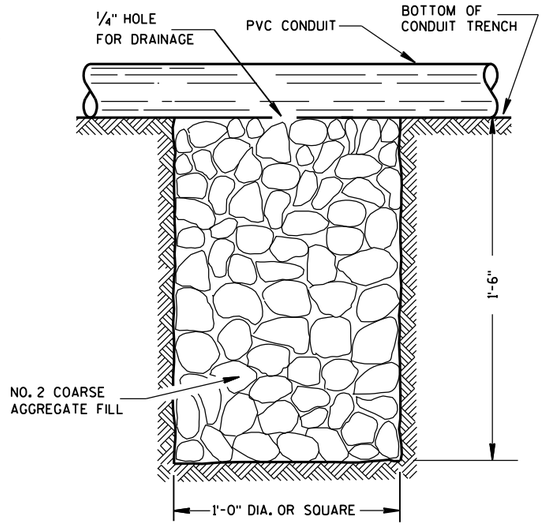
PLAN VIEW

ARROW MARK



NOTE: INSTALL AT LOCATIONS WHERE METALLIC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

DRAIN SUMP FOR METALLIC CONDUIT



NOTE: INSTALL AT LOCATIONS WHERE PVC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

DRAIN SUMP FOR PVC CONDUIT

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSION TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

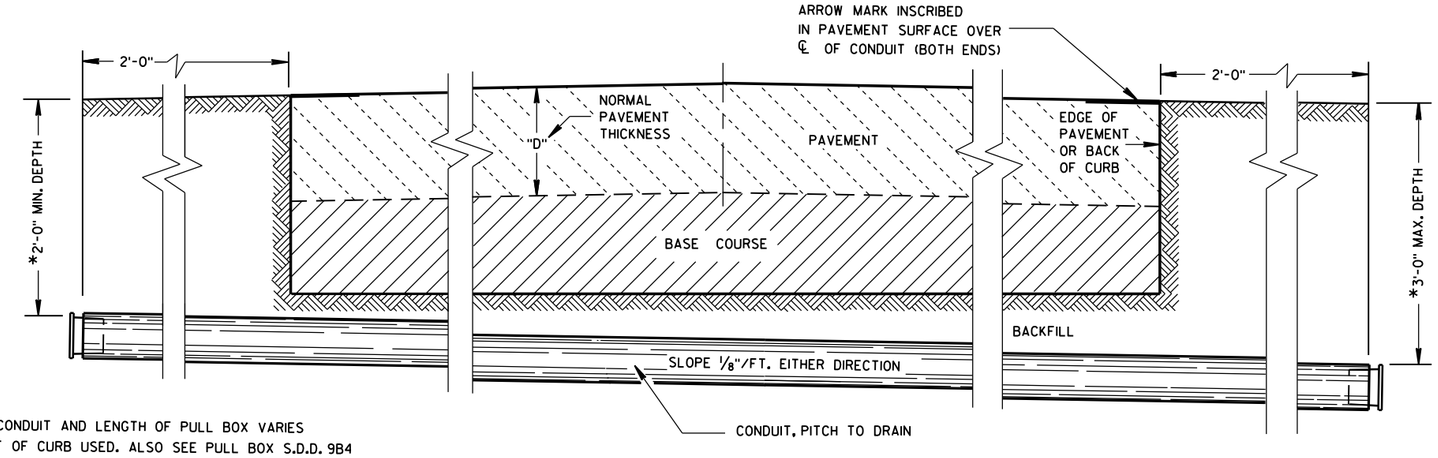
PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.



*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

SIDE ELEVATION

DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

CONDUIT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2015 — DATE
/S/ Ahmet Demirbilek — STATE ELECTRICAL
FHWA

6

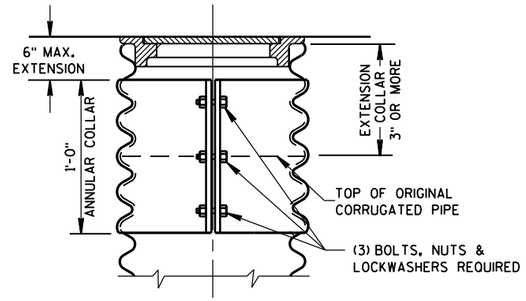
S.D.D. 9 B 2-9

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

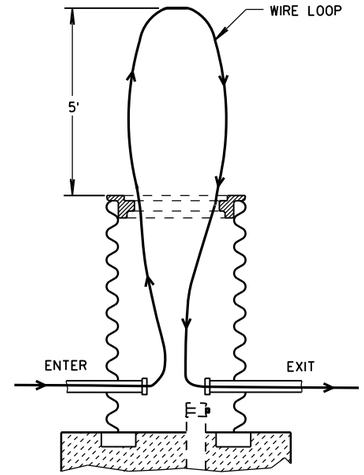
DIMENSION IN INCHES		CORRUGATED STEEL PIPE								
PIPE DIAMETER (INSIDE)	A	12	12	12	18	18	18	24	24	24
PIPE LENGTH **	B	24	30	36	24	30	36	36	42	48
WALL THICKNESS	C	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 1/4	10 1/4	10 1/4	16 1/4	16 1/4	16 1/4	22 1/4	22 1/4	22 1/4
FRAME	E	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2	26 1/2	26 1/2	26 1/2
FRAME	F	8 1/2	8 1/2	8 1/2	14 1/2	14 1/2	14 1/2	20 1/2	20 1/2	20 1/2
FRAME	G	11 1/2	11 1/2	11 1/2	17 1/2	17 1/2	17 1/2	23 1/2	23 1/2	23 1/2
WEIGHT IN POUNDS *										
FRAME AND COVER		60	60	60	110	110	110	155	155	155

* THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.

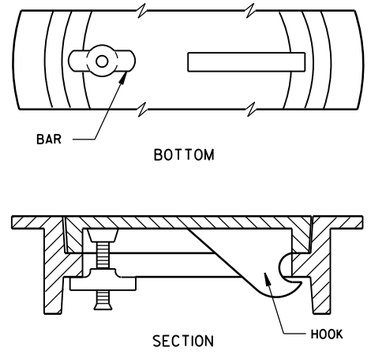
** NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.



CORRUGATED PIPE EXTENDER

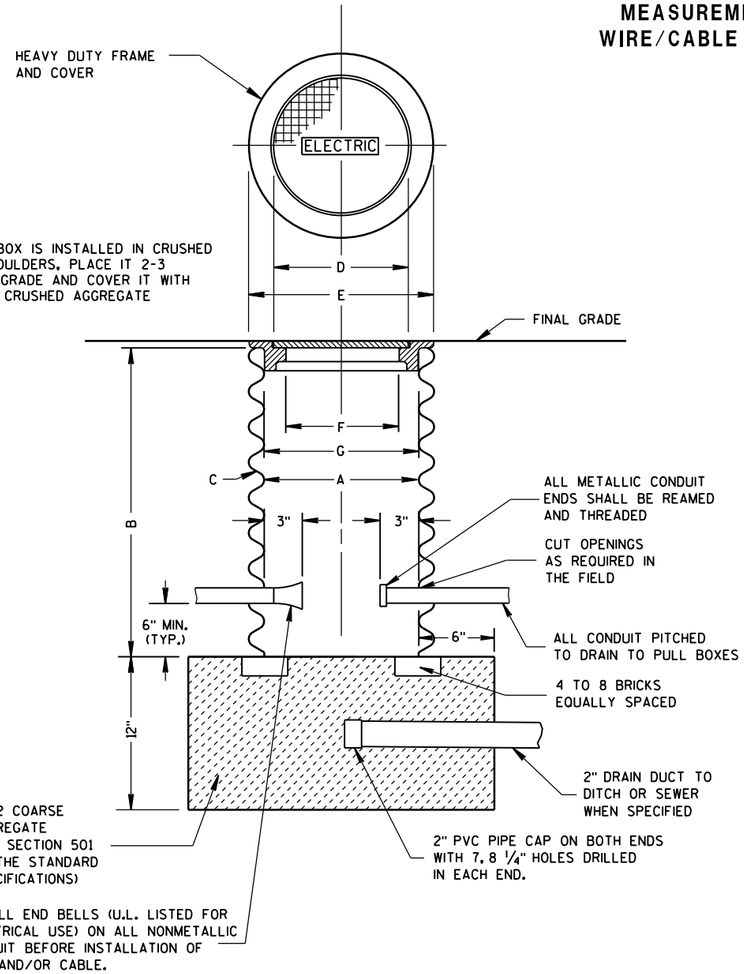


MEASUREMENT DETAIL FOR WIRE/CABLE IN THE PULL BOX



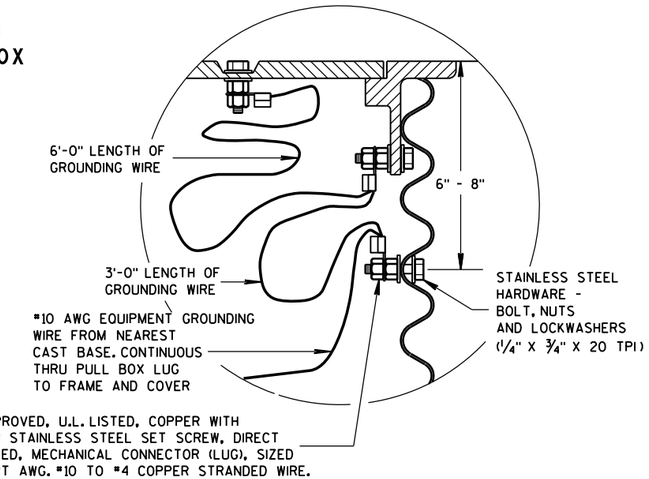
ALTERNATE COVER (LOCKING)

TIGHTENING BAR TYPE

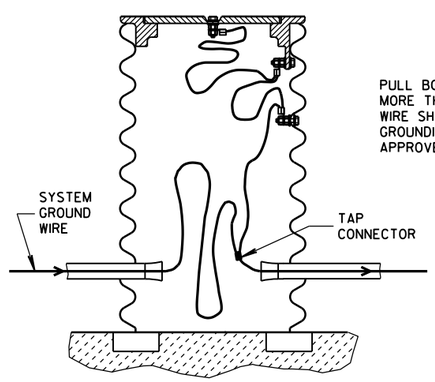


WHEN A PULL BOX IS INSTALLED IN CRUSHED AGGREGATE SHOULDERS, PLACE IT 2-3 INCHES BELOW GRADE AND COVER IT WITH 2-3 INCHES OF CRUSHED AGGREGATE

PULL BOX



EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES



PULL BOX TO NEAREST BASE DISTANCE MORE THAN 20 FEET. PULL BOX GROUND WIRE SHALL CONNECT AT SYSTEM GROUNDING WIRE. USE DEPARTMENT APPROVED TAP CONNECTOR.

EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR TRAFFIC LOADS.

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED FOR USE WITH COPPER WIRE.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.

PULL BOX

STATE OF WISCONSIN
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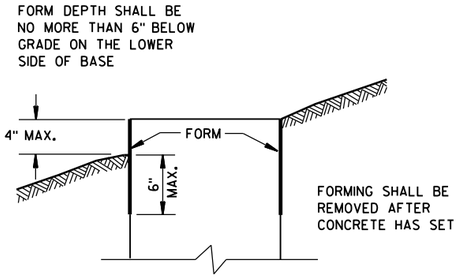


FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS.

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN AT THE ENTRANCE OF THE BASE.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6 X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

GENERAL NOTES (CONTINUED)

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION OF CABLE OR WIRE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FOR FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NONMETALLIC.

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG, STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 1, TYPE 2, TYPE 5, AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER THE BASE OF THE TYPE 2 AND TYPE 5 BASES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4" "L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND END SHALL NOT BE THREADED.

ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

(1) THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL BY THE ENGINEER.

(2) (4) 1" DIA. X 3'-6" ANCHOR RODS.

(3) (4) 1" DIA. X 5'-0" ANCHOR RODS.

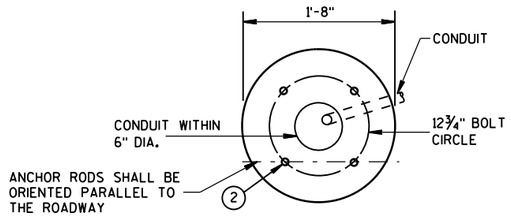
(4) (6) NO. 6 X 6'-8" BAR STEEL REINFORCEMENT.

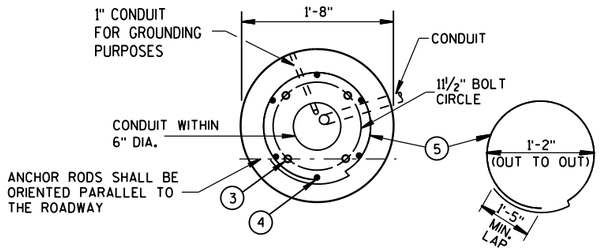
(5) (7) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.

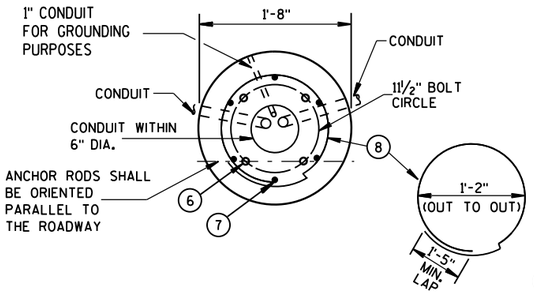
(6) (4) 1" DIA. X 3'-6" ANCHOR RODS.

(7) (6) NO.4 X 4'-8" BAR STEEL REINFORCEMENT.

(8) (5) NO. 4 X 5'-1" BAR STEEL REINFORCEMENT @ 1'-0" C-C.

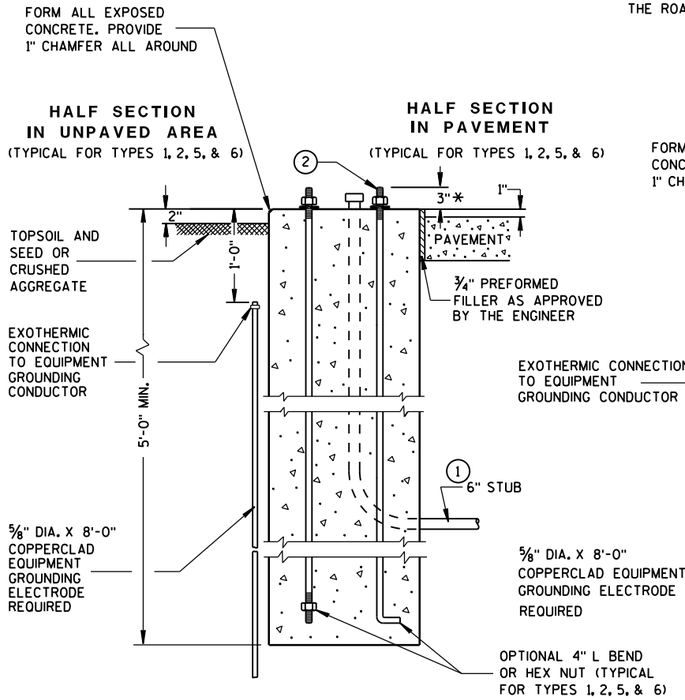


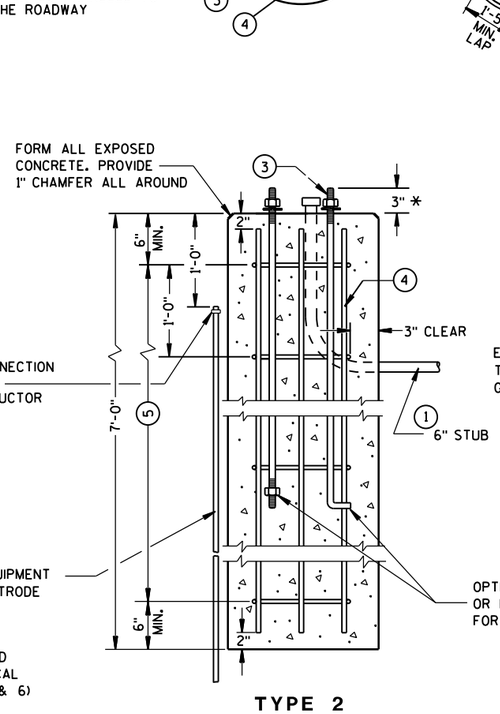




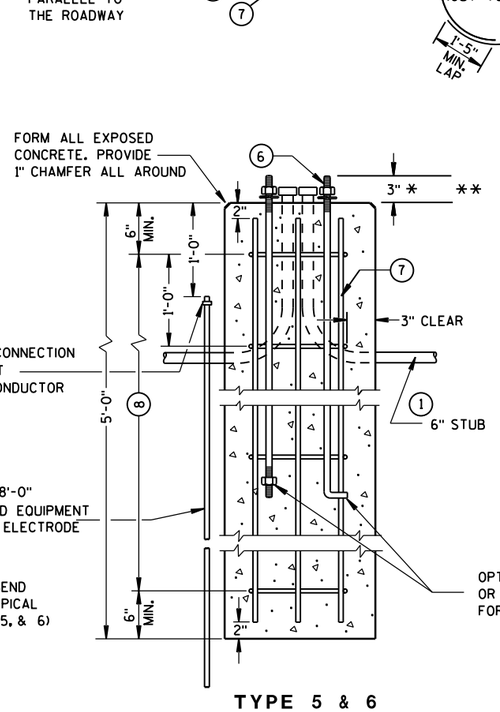
HALF SECTION IN UNPAVED AREA
(TYPICAL FOR TYPES 1, 2, 5, & 6)

HALF SECTION IN PAVEMENT
(TYPICAL FOR TYPES 1, 2, 5, & 6)





TYPE 2



TYPE 5 & 6

CONCRETE BASES

* ANY ANCHOR ROD PROJECTION SHORTER THAN 2¾" OR LONGER THAN 3¼" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

** FOR NONBREAKAWAY INSTALLATIONS, 4½" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

CONCRETE BASES, TYPES 1, 2, 5, & 6

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

FOUR (4) BOLTS SHALL BE FURNISHED WITH EACH TRANSFORMER BASE. BOLTS SHALL BE 1" DIAMETER, 4" IN LENGTH, WITH WASHERS, LOCK WASHERS AND NUTS. BOLTS, NUTS AND WASHERS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 641.2.2 OF THE STANDARD SPECIFICATIONS.

LEVELING SHIMS, IF NEEDED, SHALL BE DESIGNED FOR THE PURPOSE AND USED UNDER CAST BASES WHEN PLUMBING POLES OR STANDARDS DURING INSTALLATION. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE.

SHIM LENGTH SHALL BE LONG ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.

DOUBLE NUTTING IS NOT ACCEPTABLE FOR LEVELING OR MOUNTING PURPOSES.

A NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE SHALL BE FURNISHED AND INSTALLED IN THE PEDESTAL AND TRANSFORMER BASES.

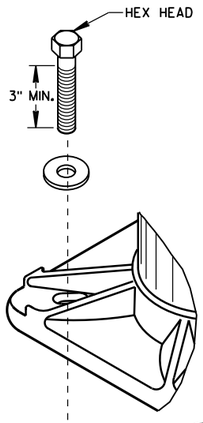
THE MECHANICAL CONNECTOR SHALL BE INSTALLED USING A 1/4" - 20 (TPI) STAINLESS STEEL HEX HEAD BOLT OF SUFFICIENT LENGTH TO FIRMLY ATTACH THE LUG TO THE BASE.

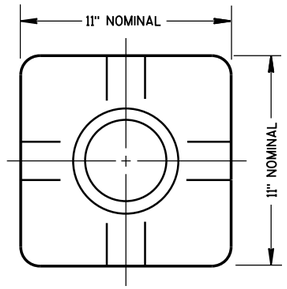
SHOULD THE MANNER OF ATTACHMENT OF THE LUG REQUIRE WASHERS, HEX NUTS, LOCK WASHER - THEY SHALL BE STAINLESS STEEL AS IS THE BOLT. THE MANNER OF ATTACHMENT SHALL NOT BLOCK ACCESSIBILITY TO WIRE PLACEMENT IN THE CONNECTOR.

PEDESTAL BASE COLLAR THREADING SHALL BE TAPERED AND IN ACCORDANCE WITH NATIONAL PIPE THREADING DIMENSIONS.

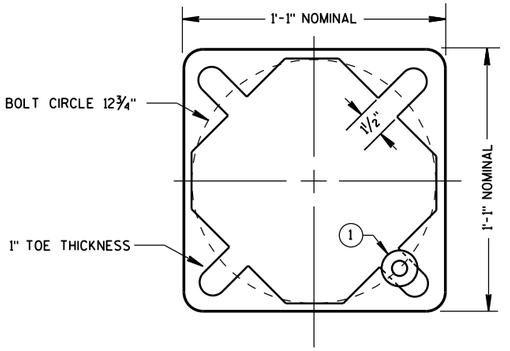
BASE COLLAR THREADING SHALL EXTEND INTO THE BASE COLLAR WITH SUFFICIENT DEPTH TO ACCEPT THE INSTALLATION OF TRAFFIC SIGNAL STANDARDS TO A DEPTH OF 1 1/2", THEN TIGHTENING TO A POINT OF BEING IMMOVABLE.

THE ACCESS DOOR SHALL BE OF THE SAME MATERIAL AS THE BASE.

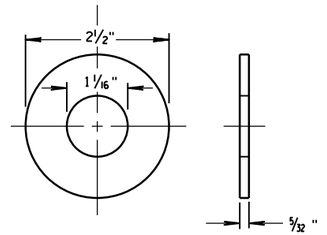




TOP VIEW
(PEDESTAL BASE)

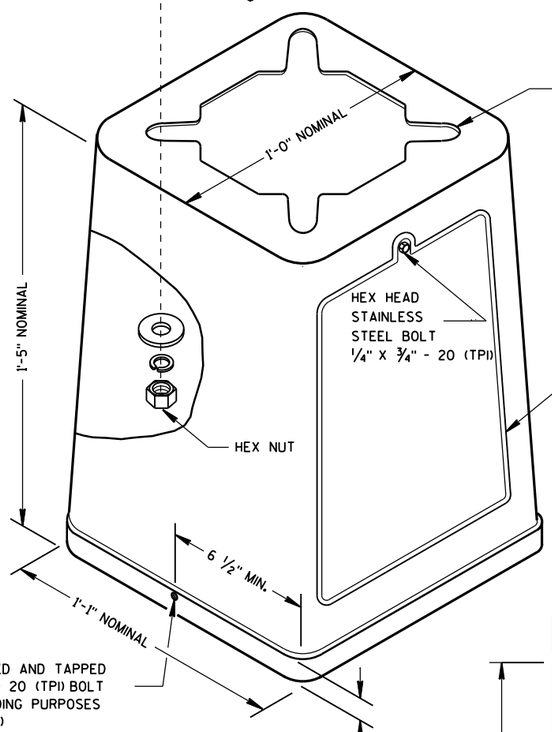


BOTTOM VIEW
(PEDESTAL BASE)

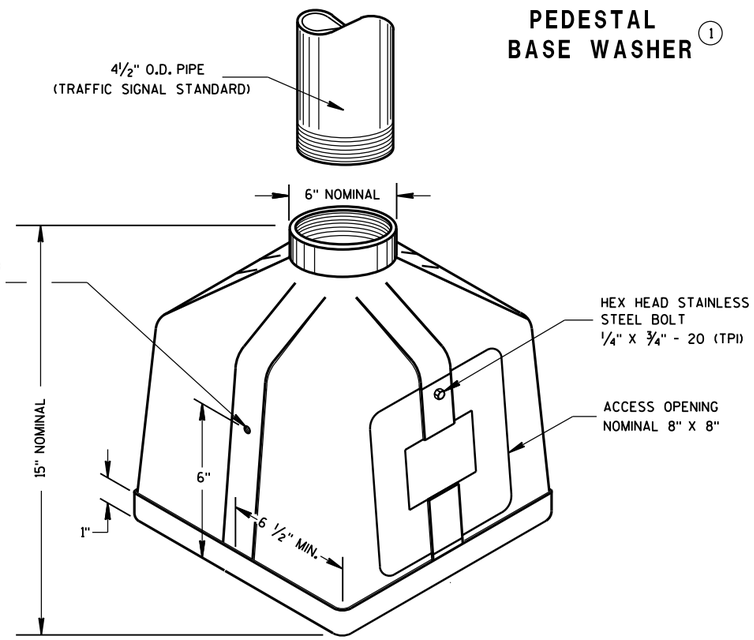


ZINC COATED STEEL WASHER
TO BE PROVIDED BY THE CONTRACTOR

PEDESTAL BASE WASHER (1)

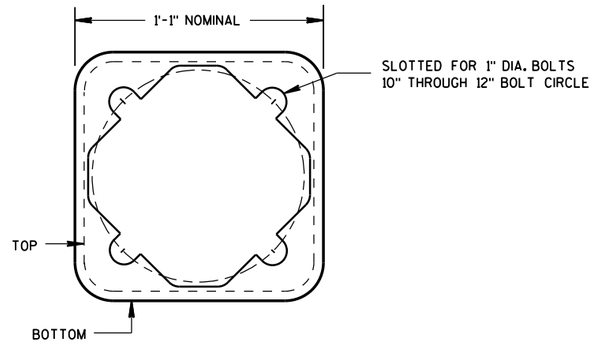


ISOMETRIC VIEW

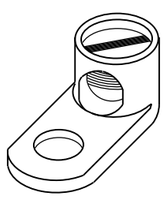


ISOMETRIC VIEW

PEDESTAL BASE



BOTTOM VIEW
(TRANSFORMER BASE)

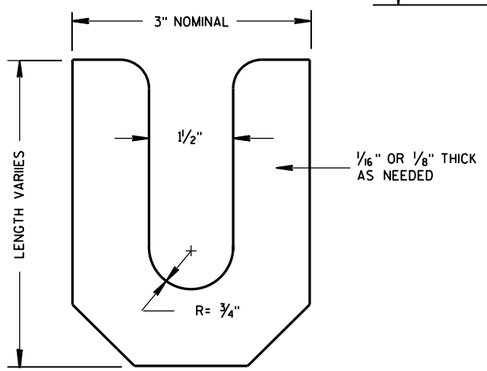


TYPICAL MECHANICAL CONNECTOR LUG

TO BE FURNISHED WITH EACH BASE

TRANSFORMER BASE

INTENDED FOR USE WITH TYPE 2, 3, 4, 5 & 6 POLES



LEVELING SHIM

TRANSFORMER/PEDESTAL BASES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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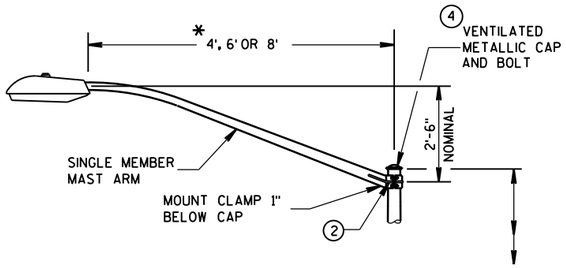
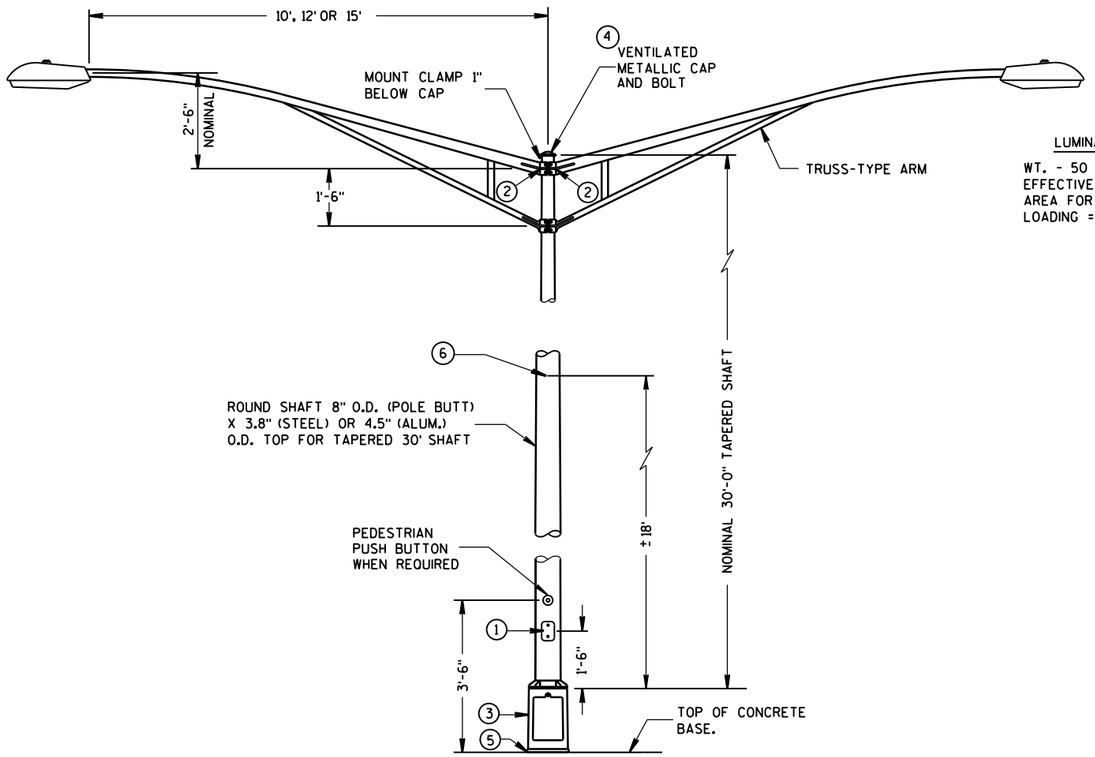
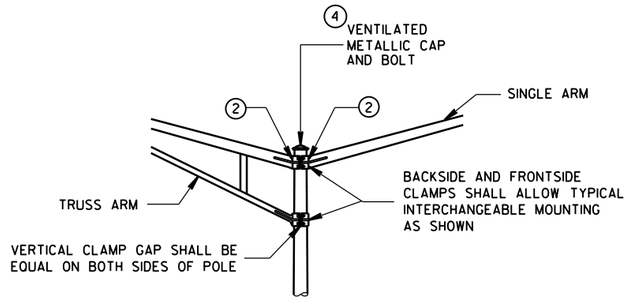
*RISE FOR 4' ARM SHALL BE 2'-0".





TYPE 5 POLE MOUNTING CONFIGURATION
(MAXIMUM LOAD)
LIGHTING ONLY



INTERCHANGEABLE MOUNTING DETAIL

LUMINAIRE

WT. - 50 LBS.
EFFECTIVE PROJECTED
AREA FOR WIND
LOADING = 1.5 SQ. FT.

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL TYPE 5 POLE MOUNTINGS SHALL BE DESIGNED TO INCLUDE TWIN 15' ARMS WITH LUMINAIRES.

POLES SHALL BE GALVANIZED STEEL OR ALUMINUM, AS CALLED FOR IN THE CONTRACT.

TYPE 5 ALUMINUM POLES SHALL BE CONSTRUCTED OF 6063-T6 ALUMINUM ALLOY. SLEEVING INSIDE THE POLE IS NOT ACCEPTABLE.

THE TYPE 5 ALUMINUM POLES SHALL HAVE A MINIMUM WALL THICKNESS OF 0.188".

TYPE 5 STEEL POLES SHALL HAVE A MINIMUM WALL THICKNESS OF U.S. STANDARD 11 GAGE (.1196").

THE SLIPFITTER END OF THE LUMINAIRE MAST ARM SHALL BE A NOMINAL 2⅜ INCHES IN OUTSIDE DIAMETER. THE STRAIGHT PORTION OF THE SLIPFITTER END OF THE LUMINAIRE ARM SHALL BE A NOMINAL 12 INCHES IN LENGTH.

WHEN TRANSFORMER BASES ARE USED, WIRE CONEECTIONS SHALL BE MADE IN THE TRANSFORMER BASE.

(1) 4" x 6" REINFORCED HANDHOLE & COVER ASSEMBLY WITH 2 (TWO) ¼" X ¾" - 20 TPI HEX HEAD STAINLESS STEEL BOLTS.

(2) GROMMETS, 1" CHASE NIPPLES OR 1" CLOSE CONDUIT NIPPLES WITH BUSHINGS SHALL BE PROVIDED FOR 1⅜" HOLE IN POLE SHAFT FOR WIRING.

(3) CAST ALUMINUM TRANSFORMER BASE, WHEN REQUIRED.

(4) FURNISH AND INSTALL VENTILATED, CAST, METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) ¼" X ¾" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.

(5) SHIMMING, IF NEEDED, SHALL BE LOCATED BETWEEN THE CONCRETE FOUNDATION AND THE TRANSFORMER BASE.

(6) INTERNAL DUMBBELL-TYPE VIBRATION DAMPER.

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POLE MONTINGS FOR
LIGHTING UNITS, TYPE 5
(30 FEET)

STATE OF WISCONSIN
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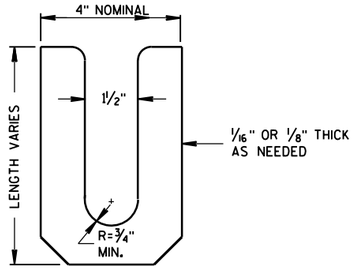
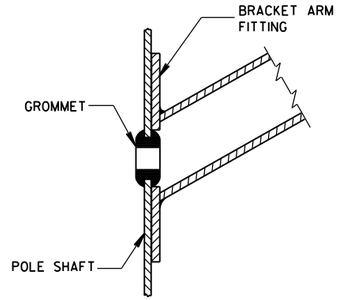
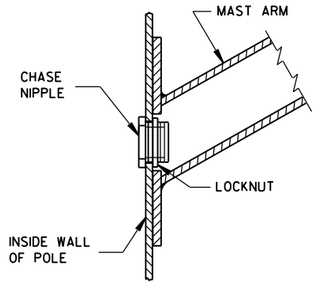


LEVELING SHIM (13)

SHALL BE ALUMINUM



TYPICAL APPLICATION OF GROMMET IN POLE SHAFT



TYPICAL APPLICATION OF CHASE NIPPLE IN POLE SHAFT

GENERAL NOTES

CLAMP BOLT-NUT TIGHTENING TORQUE SHALL BE INDICATED BY INDENT STAMPING (1/2 INCH NUMERALS AND LETTERS) OR WEATHERPROOF PRINTING ON THE INSIDE OF THE CLAMP THAT IS WELDED TO THE ARM MEMBER.

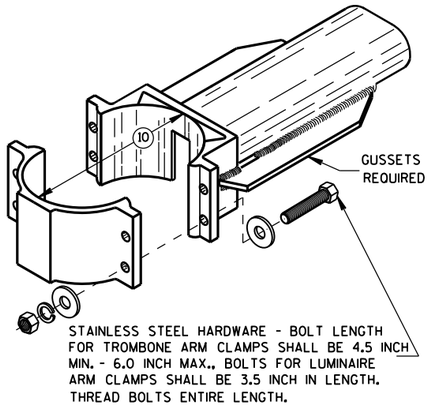
(10) 4.5" I.D. FOR LUMINAIRE MAST ARM CLAMP.
6.625" I.D. FOR TROMBONE MAST ARM CLAMP.

(11) INDIVIDUAL BASE PLATE ANCHOR ROD COVERS. (4 REQUIRED)

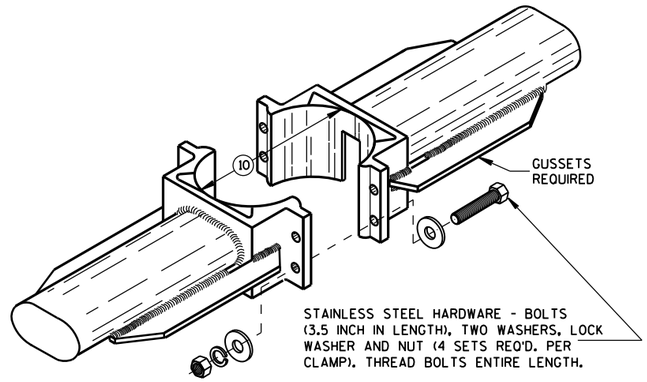
(12) BASE PLATE SLOTTED TO ACCEPT 11" THROUGH 12" BOLT CIRCLE USING 1" DIAMETER ANCHOR RODS.

(13) LEVELING SHIMS, DESIGNED FOR THE PURPOSE, SHALL BE USED WHEN PLUMBING POLES. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE. LEVELING SHIMS SHALL BE USED ONLY BETWEEN THE TOP OF THE CONCRETE BASE AND A METALLIC BASE PLATE.

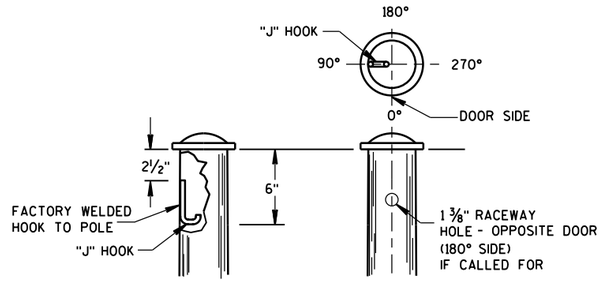
SHIMS SHALL BE LONG ENOUGH AND WIDE ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.



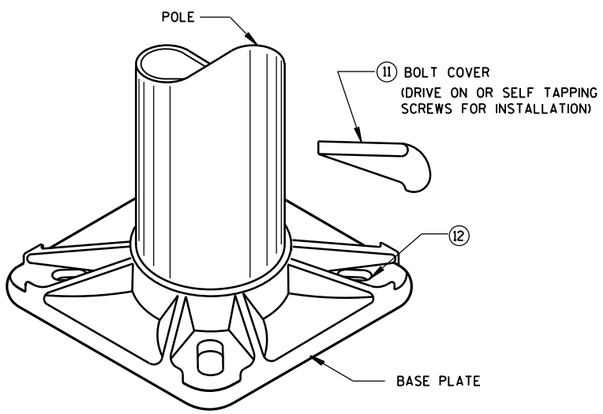
TYPICAL TROMBONE MAST ARM AND SINGLE LUMINAIRE MAST ARM MOUNTING CLAMP



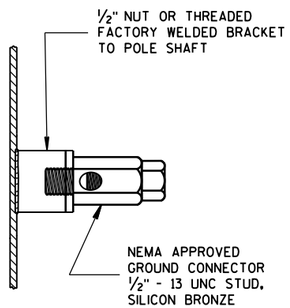
TYPICAL LUMINAIRE MAST ARM (DOUBLE) MOUNTING BRACKETS

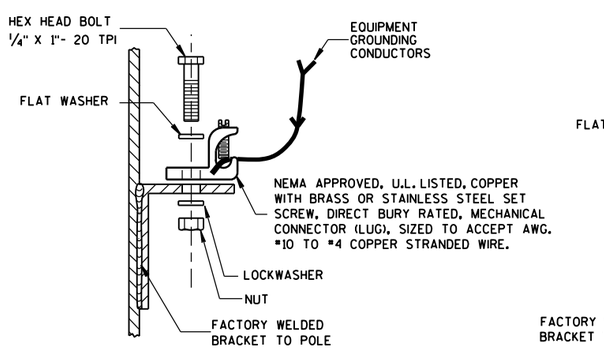


TYPICAL "J" HOOK LOCATION



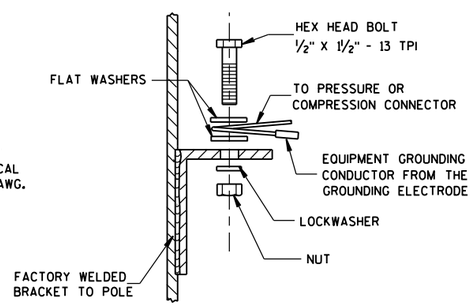
BASE PLATE





TYPICAL GROUNDING CONNECTIONS

NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL



HARDWARE DETAILS FOR POLE MOUNTINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Feb. 2015
DATE

/S/ Ahmet Demirbilek
STATE ELECTRICAL

FHWA

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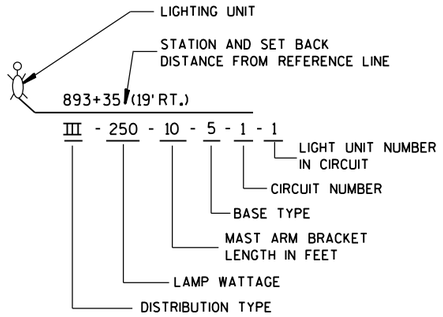
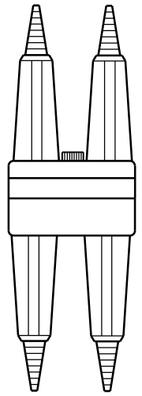
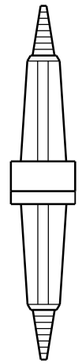
S.D.D. 9 E 1-14g



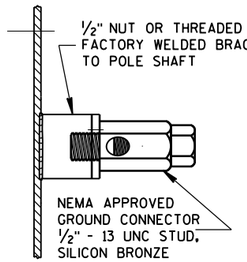
LIGHTING UNIT CODE
(TYPICAL)

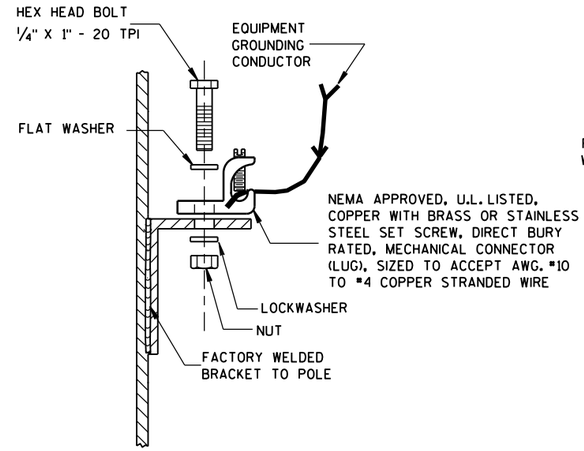


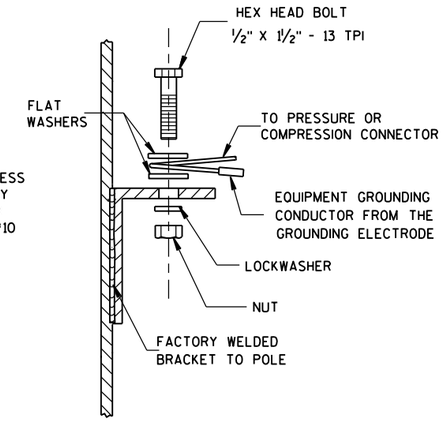
DETAIL "A"
BREAKAWY
DOUBLE POLE WITH
WATERPROOF
INSULATING BOOT



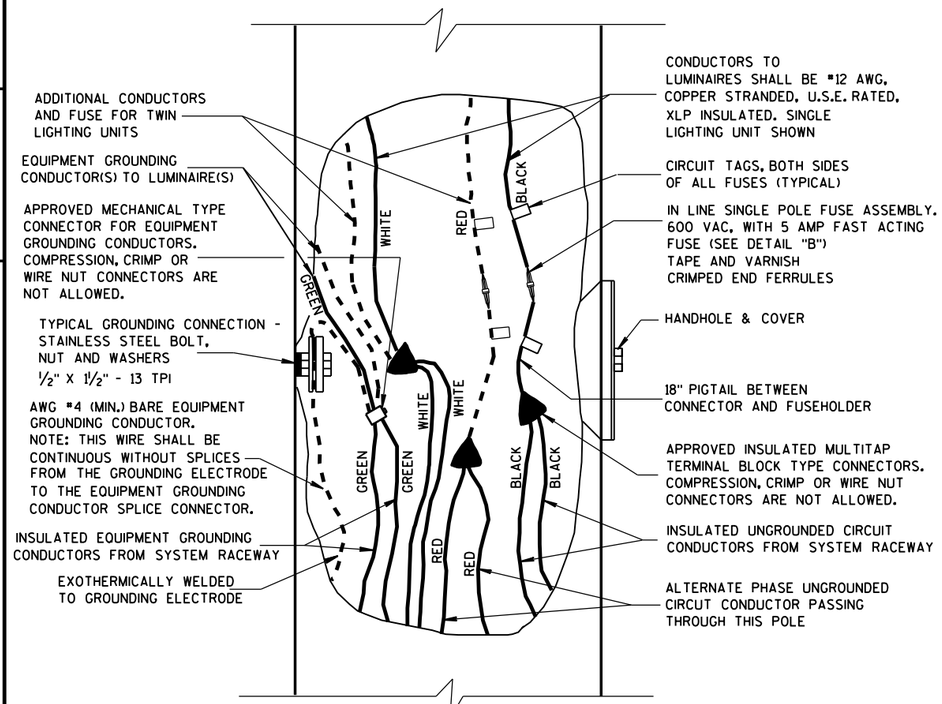
DETAIL "B"
BREAKAWY
SINGLE POLE WITH
WATERPROOF
INSULATING BOOT



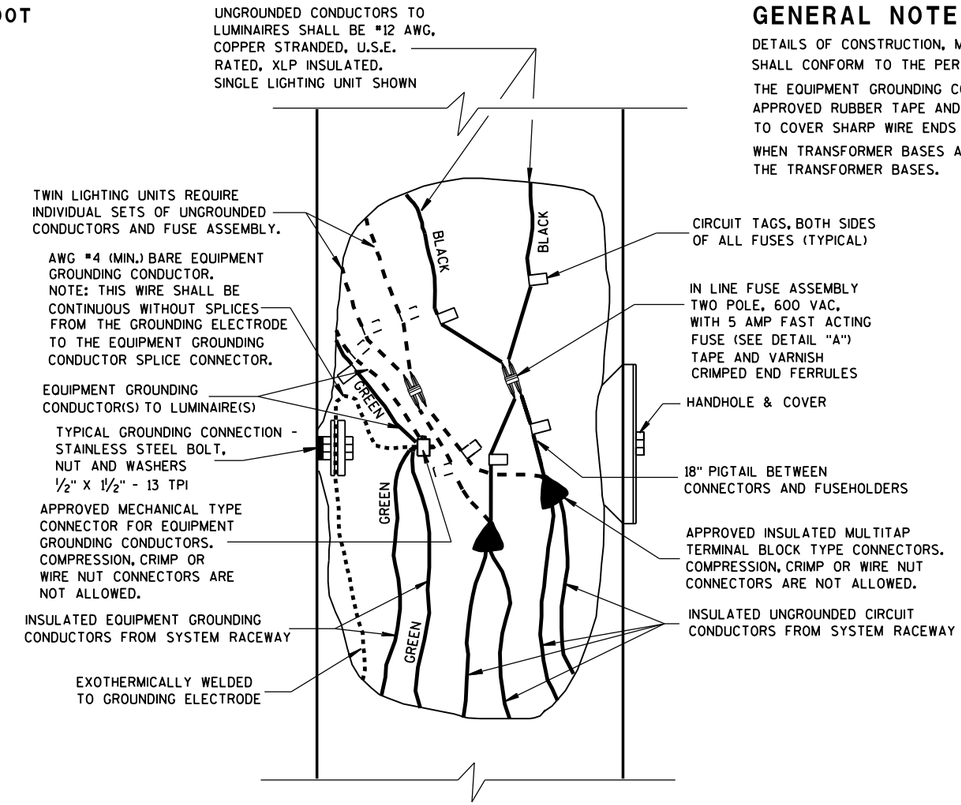




TYPICAL GROUNDING CONNECTIONS
NUT, BOLT, WASHERS AND LOCKWASHERS SHALL BE STAINLESS STEEL



3 WIRE - 120, 240 OR 480 VAC (UNGROUNDED CONDUCTOR) WITH GROUNDED CONDUCTOR AND WITH EQUIPMENT GROUNDING CONDUCTOR



2 WIRE - 240 OR 480 VAC (UNGROUNDED CONDUCTORS) WITH EQUIPMENT GROUNDING CONDUCTOR

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

THE EQUIPMENT GROUNDING CONNECTOR SHALL BE TAPED WITH 3 WRAPS (MINIMUM) OF APPROVED RUBBER TAPE AND THEN 3 WRAPS (MINIMUM) OF APPROVED VINYL TAPE TO COVER SHARP WIRE ENDS AFTER THE CONNECTION IS COMPLETED.

WHEN TRANSFORMER BASES ARE USED, ALL WIRING CONNECTIONS SHALL OCCUR WITHIN THE TRANSFORMER BASES.

NON-FREEWAY LIGHTING UNIT POLE WIRING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2014
DATE
/S/ Ahmet Demirbilek
STATE ELECTRICAL

FHWA

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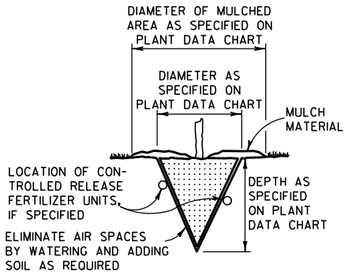
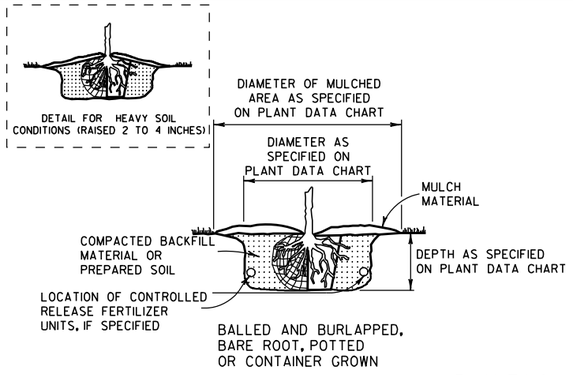
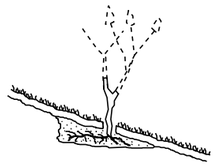
S.D.D. 9 E 3-5



MACHINE MOVED

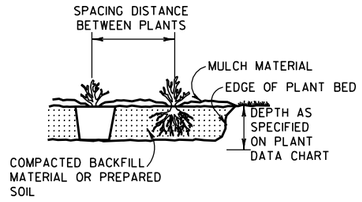


BALLED AND BURLAPPED, BARE ROOT, POTTED OR CONTAINER GROWN



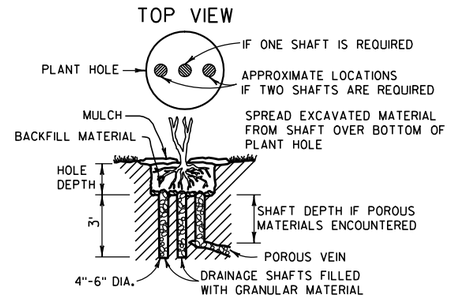
ACCOMMODATE ROOTS (SMOOTH AND STAGHORN SUMAC)

NOTE:
1) ENGINEER SHALL REQUIRE 3 SLITS IN POT TO SPEED DETERIORATION
2) METAL, PLASTIC OR OTHER NONDEGRADABLE POTS SHALL BE REMOVED PRIOR TO PLANTING



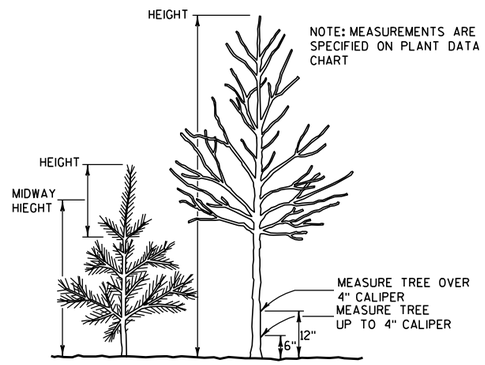
PREPARED BED (BARE ROOT AND POTTED)

PLANTING

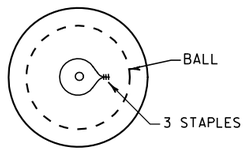


NOTE:
DRAINAGE SHAFT AS SPECIFIED ON PLANT DATA CHART

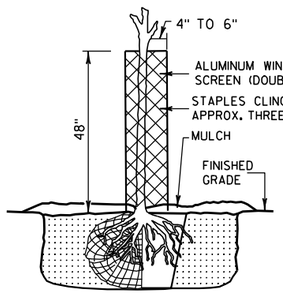
DRAINING



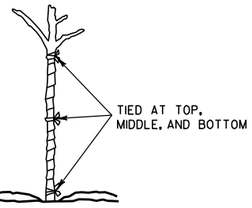
MEASURING



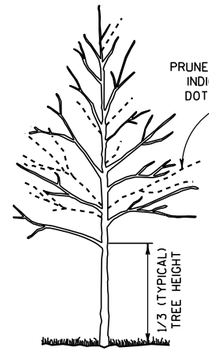
TOP VIEW



RODENT PROTECTION



WRAPPING



TALL DECIDUOUS TREES (ASH, MAPLE, LINDEN, OAK)

SINGLE STEMMED LOW DECIDUOUS TREES (CRABS AND OTHER SINGLE STEM TYPE)

MULTI-STEMMED LOW DECIDUOUS TREES OR LARGE SHRUBS (HAWTHORN, SERVICEBERRY)

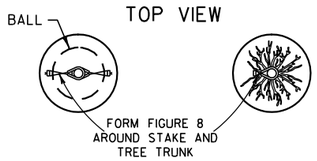
NOTE: WHEN PRUNING, PRESERVE CHARACTER AND SHAPE OF TREE. AVOID LEAVING STUBS - REMOVE BRANCH OR TWIG BACK TO THE NEAREST CROTCH
1) PRUNE TO REMOVE DEAD AND BROKEN BRANCHES
2) PRUNE TO REMOVE BRANCHES THAT TOUCH OR ARE TOO CLOSE TO OTHER BRANCHES

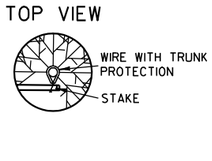
SHRUBS (GRAY DOGWOOD ARROWWOOD VIBURNUM)

SUMAC

TREE TYPE EVERGREENS (PINE, SPRUCE, FIR) EVERGREENS USUALLY ARE NOT PRUNED

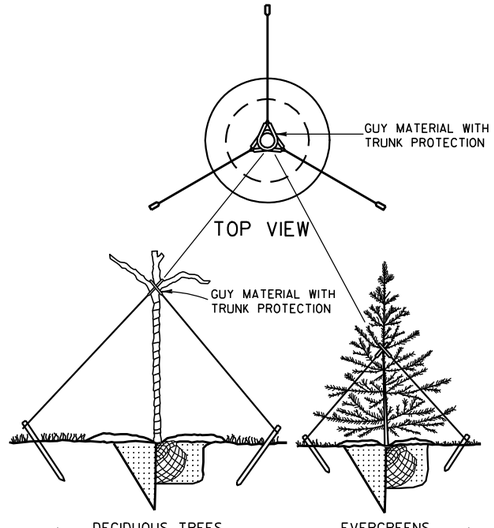
PRUNING





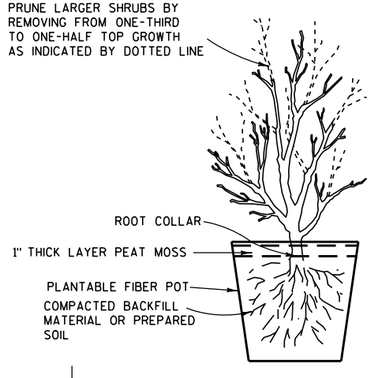
NOTE: BRACING STAKE
1) SHALL BE DRIVEN INTO THE GROUND AS CLOSE TO THE TREE AS POSSIBLE WITHOUT DAMAGING THE BRANCHES.
2) MAY BE DRIVEN AT SUCH AN ANGLE THAT IT DOES NOT PENETRATE THE BALL OR POT.
3) SHALL NOT PROTRUDE ABOVE THE TOP OF THE TREE; AND
4) SHALL HAVE A HOLE NEAR THE TOP TO HOLD THE WIRE IN PLACE.

BRACING



GUYING

PRUNE LARGER SHRUBS BY REMOVING FROM ONE-THIRD TO ONE-HALF TOP GROWTH AS INDICATED BY DOTTED LINE



POTTING

NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

BRACING, WRAPPING, GUYING, RODENT PROTECTION, FERTILIZER AND MULCH SHALL BE USED ONLY WHEN SPECIFIED ON THE PLANT DATA CHART (PART OF PLAN) OR SPECIAL PROVISIONS.

TREE PLANTING DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/11/94
DATE
/S/ Rory L. Rhinesmith
CHIEF METHODS DEVELOPMENT ENGINEER
FHWA

S.D.D. 14 A 2-1

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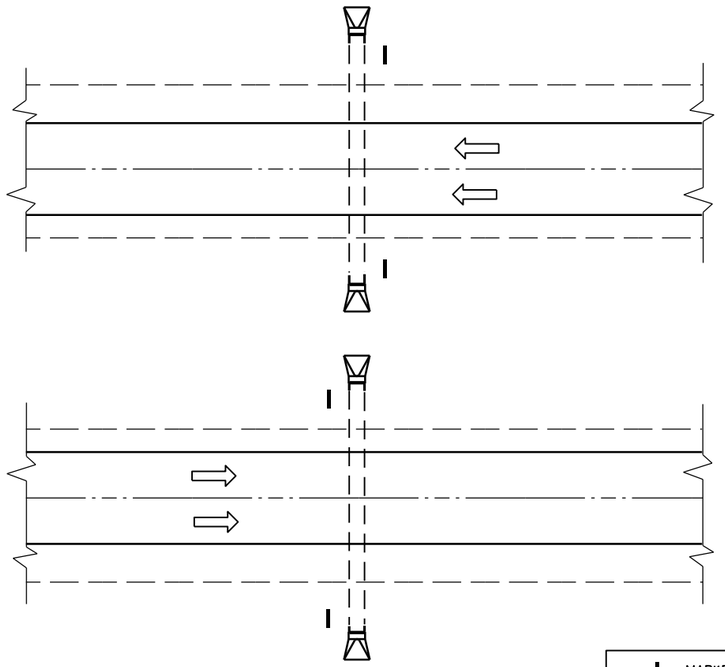
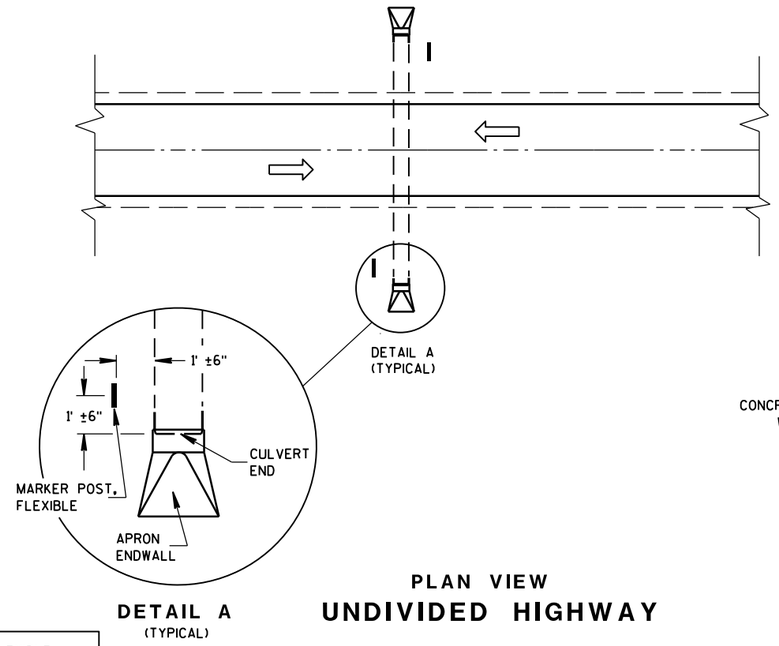
S.D.D. 14 A 2-1



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

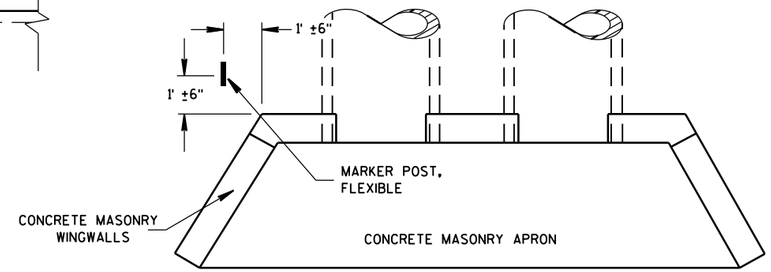
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

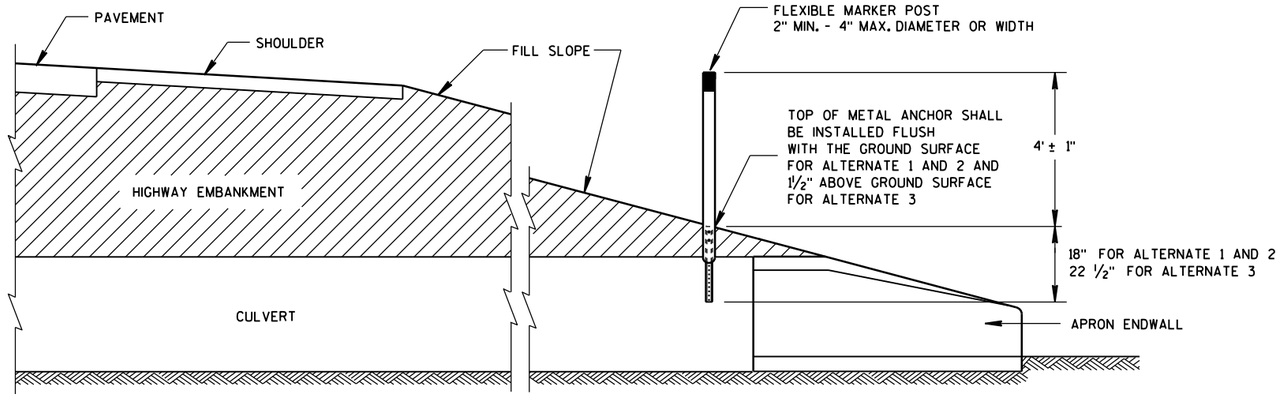
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 15 A 3-2a

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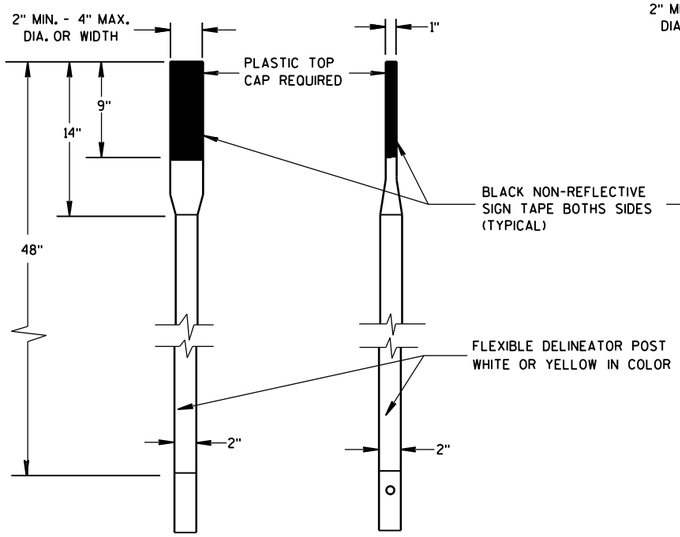
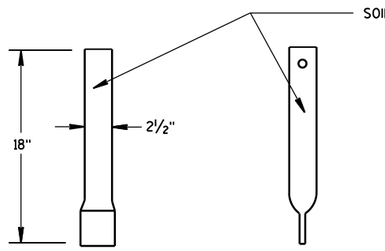
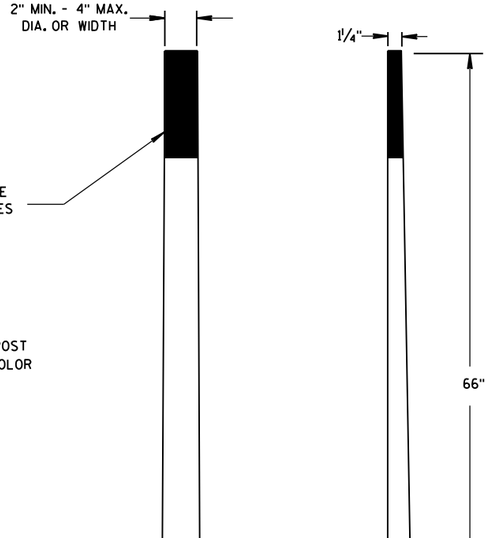
S.D.D. 15 A 3-2a





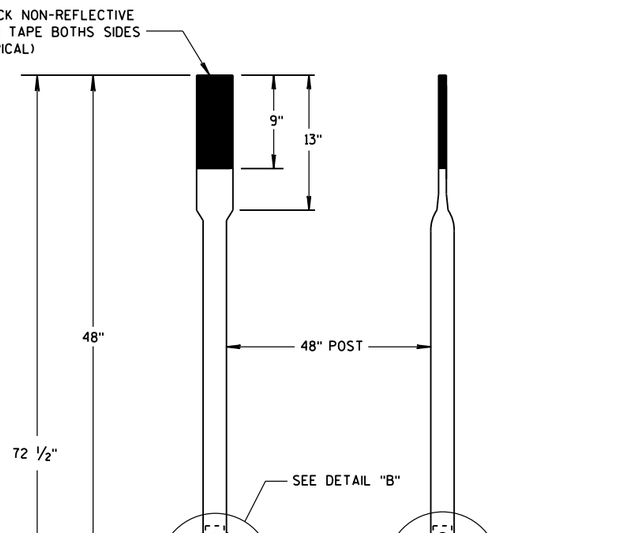
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

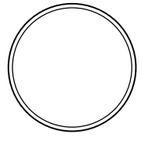
ALTERNATE 2



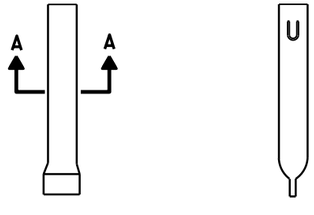
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POSTS

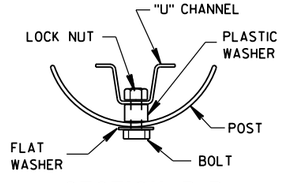


SECTION A-A

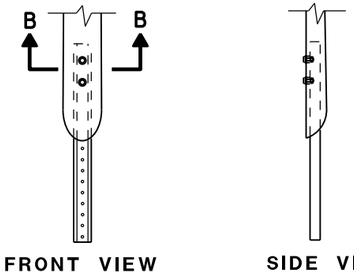


FRONT VIEW SIDE VIEW

ALTERNATE 1

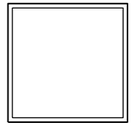


SECTION B-B

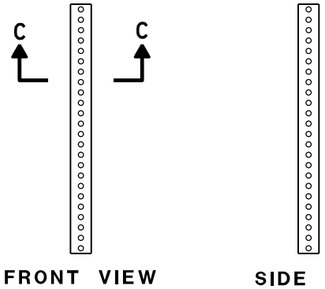


FRONT VIEW SIDE VIEW

ALTERNATE 2



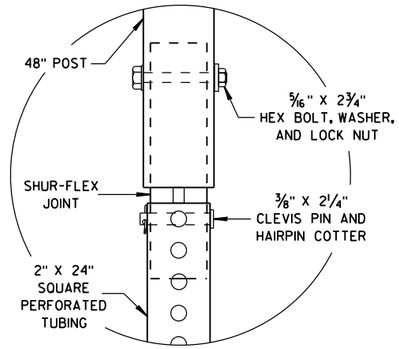
SECTION C-C



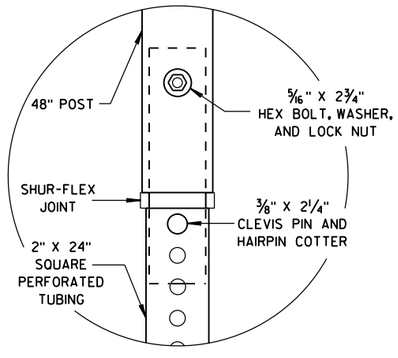
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS



DETAIL B



DETAIL C

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 A 3-2b

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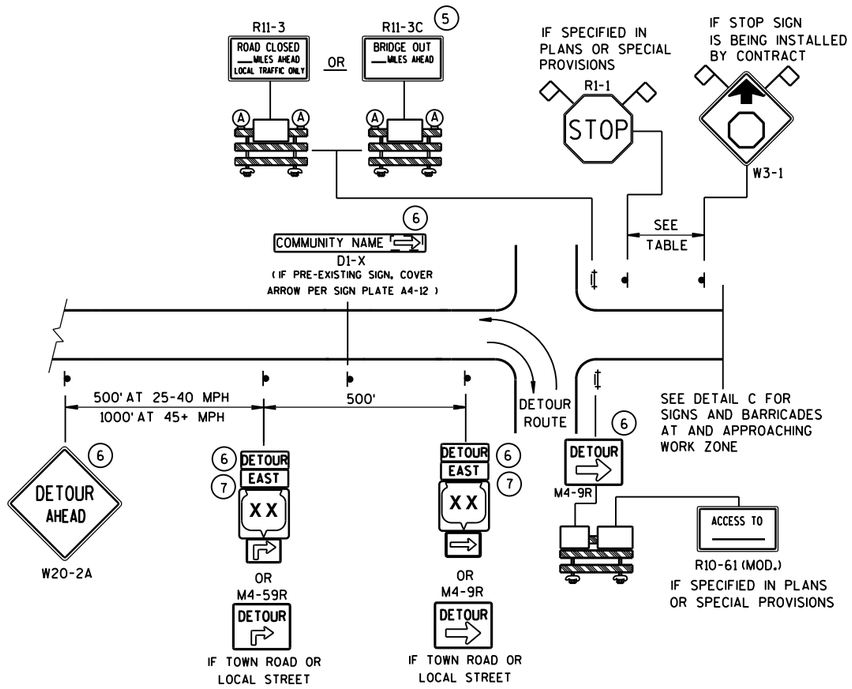
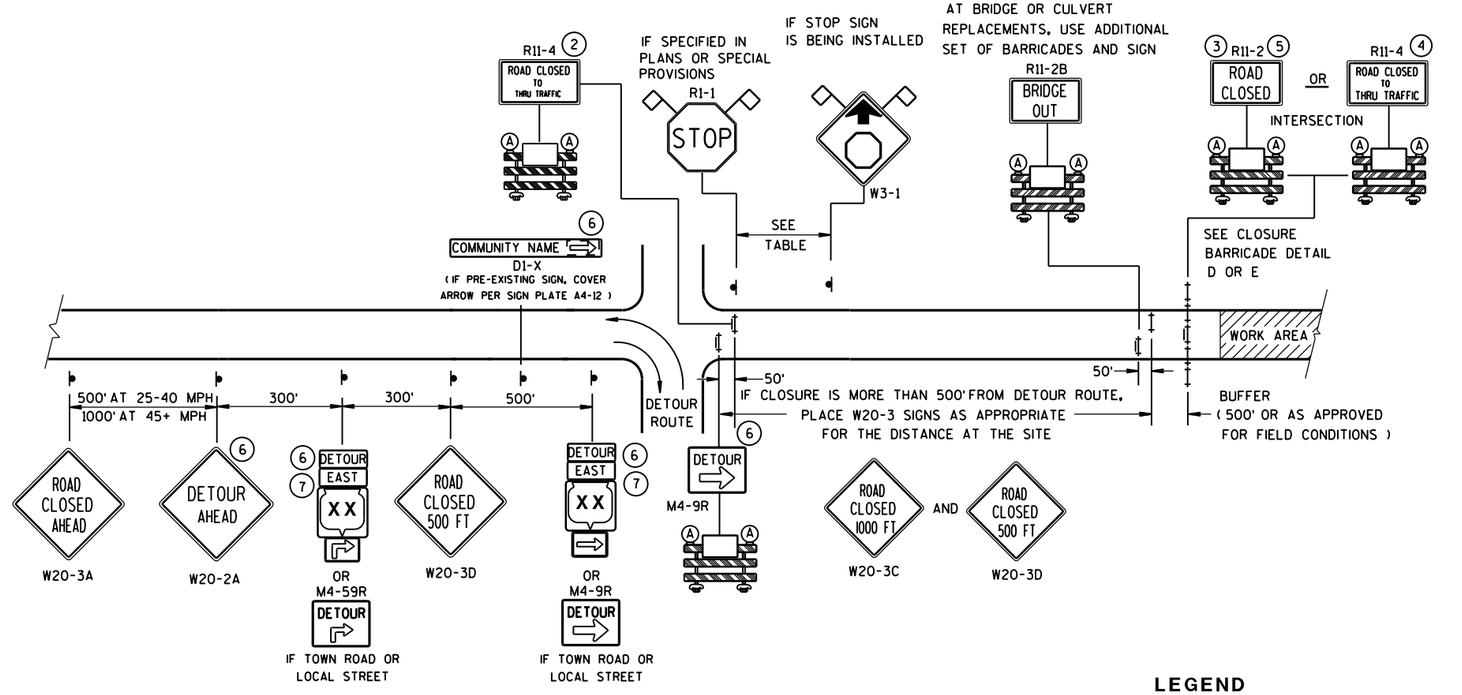
S.D.D. 15 A 3-2b



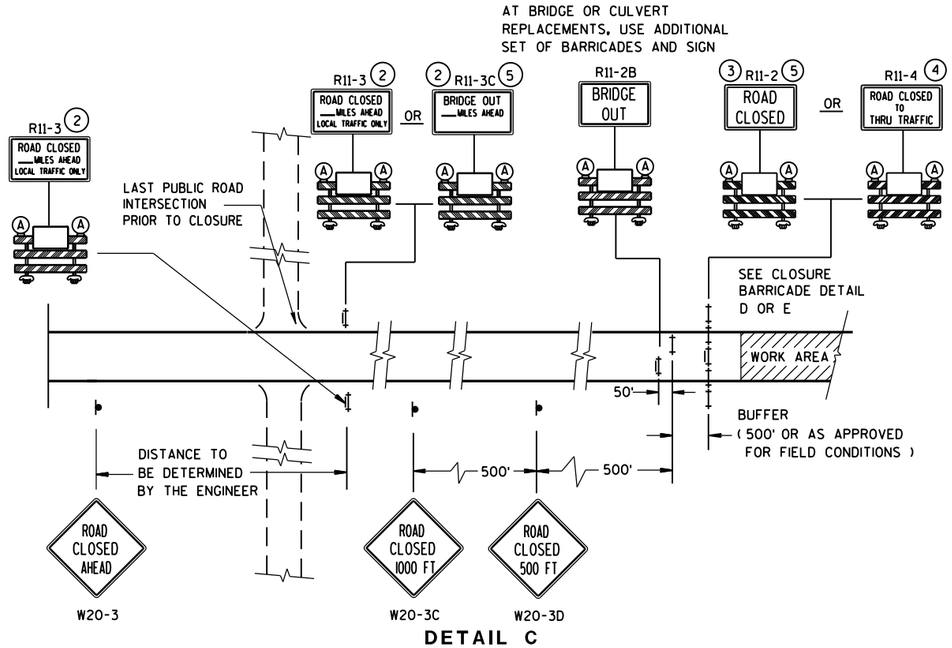
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "**b**"
FOR GENERAL NOTES
AND FOOTNOTES (1) THROUGH (7)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA

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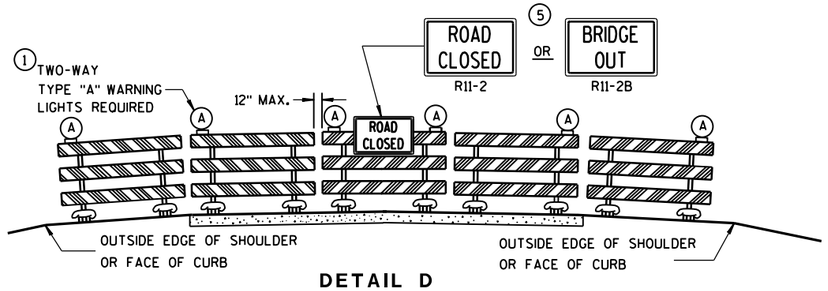
S.D.D. 15 C 2-6a

6

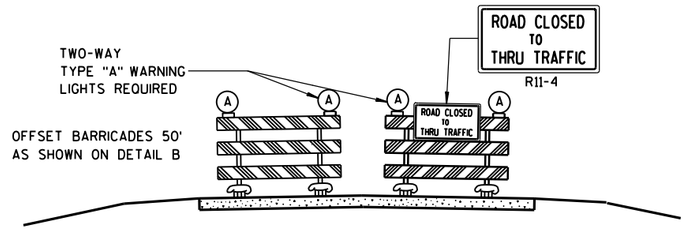
S.D.D. 15 C 2-6a



DETAIL D

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E

LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 — DATE

/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO SAFETY ENGI — FIC
FHWA

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S.D.D. 15 C 2-6b

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

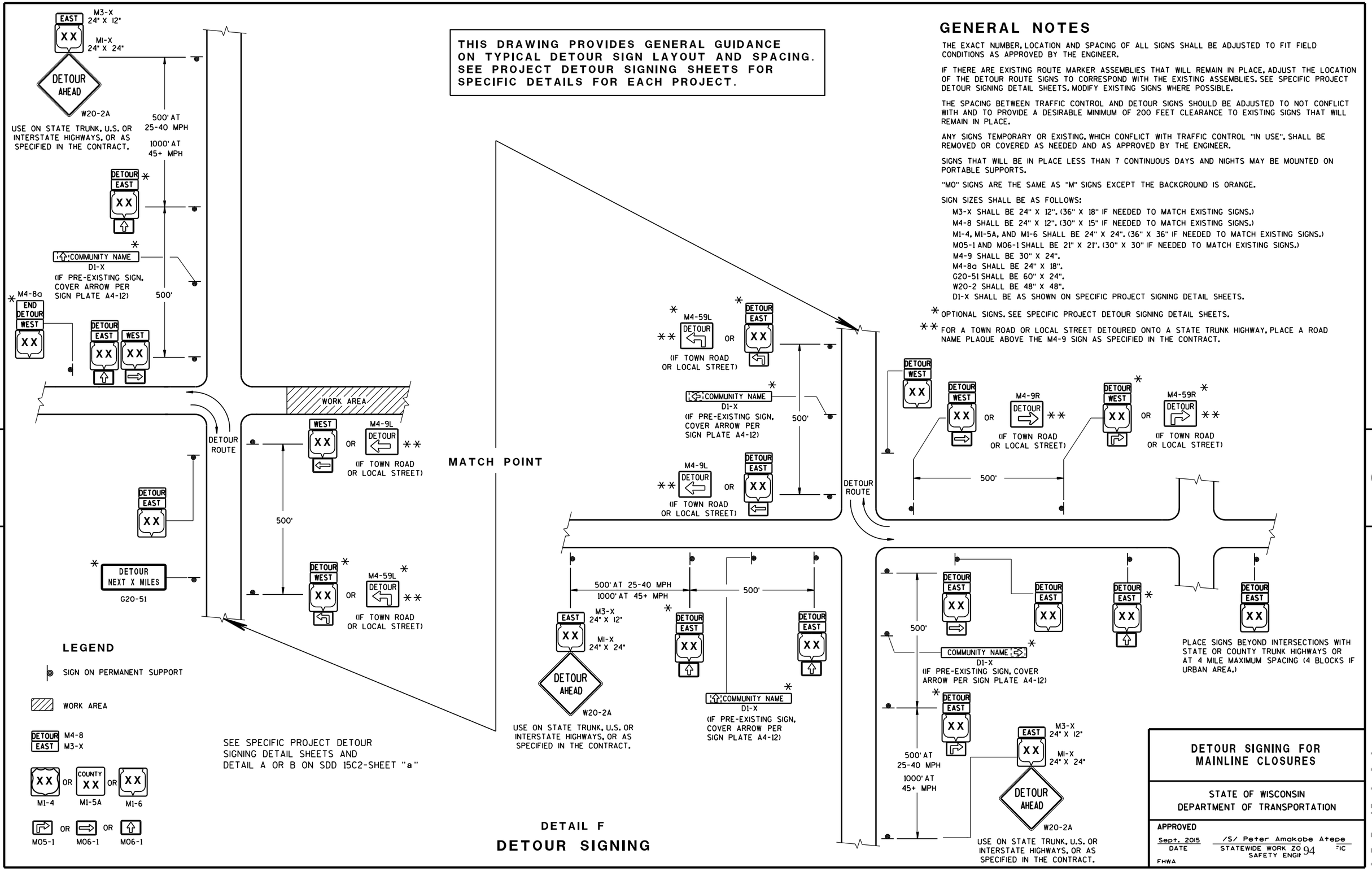
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-9 SHALL BE 30" X 24".
- M4-8a SHALL BE 24" X 18".
- G20-51 SHALL BE 60" X 24".
- W20-2 SHALL BE 48" X 48".
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR EAST — M4-8 / M3-X
- M1-4 OR M1-5A (COUNTY) OR M1-6
- M05-1 OR M06-1 OR M06-1

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD 15C2-SHEET "a"

DETAIL F

DETOUR SIGNING

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

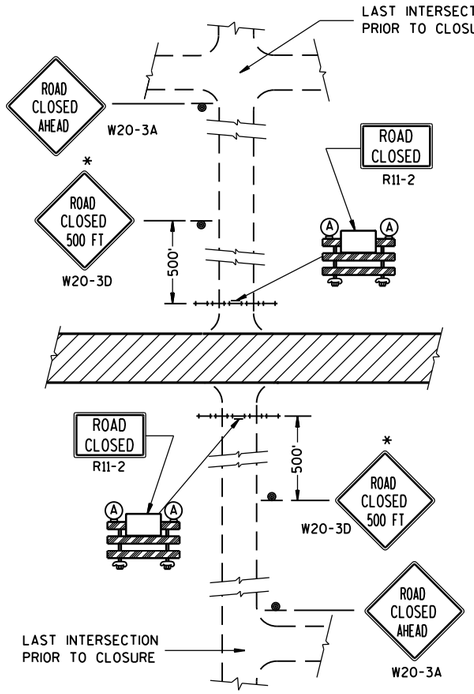
APPROVED

Sept. 2015 — DATE

/S/ Peter Amakobe Atepe — STATEWIDE WORK ZONE SAFETY ENGINEER

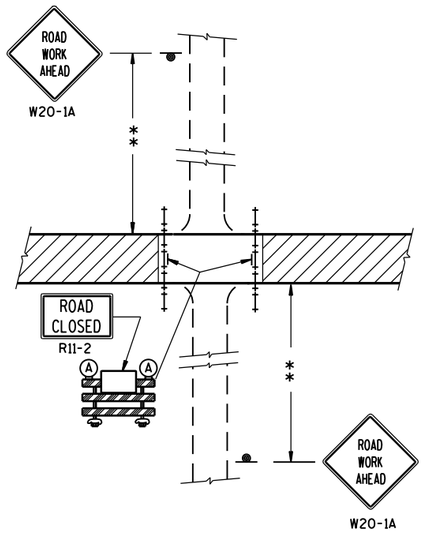
FHWA

S.D.D. 15 C 2-6c



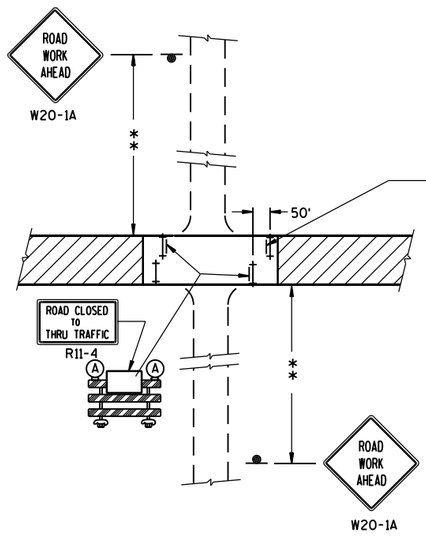
DETAIL 1

(NO ACCESS TO PROJECT)



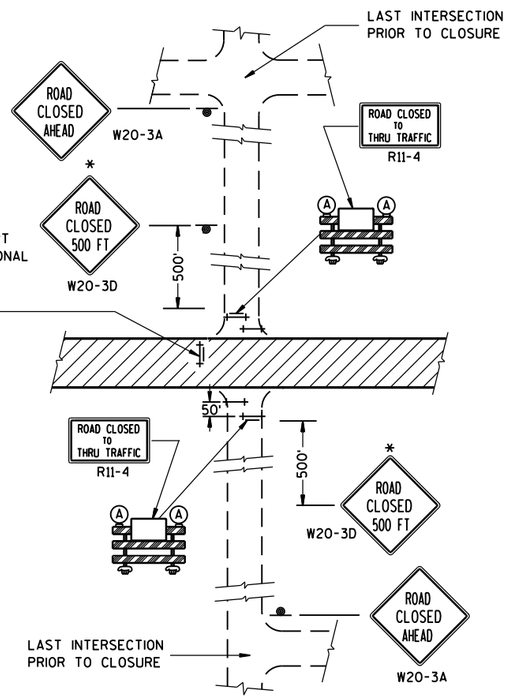
DETAIL 2

(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT).



DETAIL 3

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR,
LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2015 — DATE
/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO... SAFETY ENGI... — FHWA

6

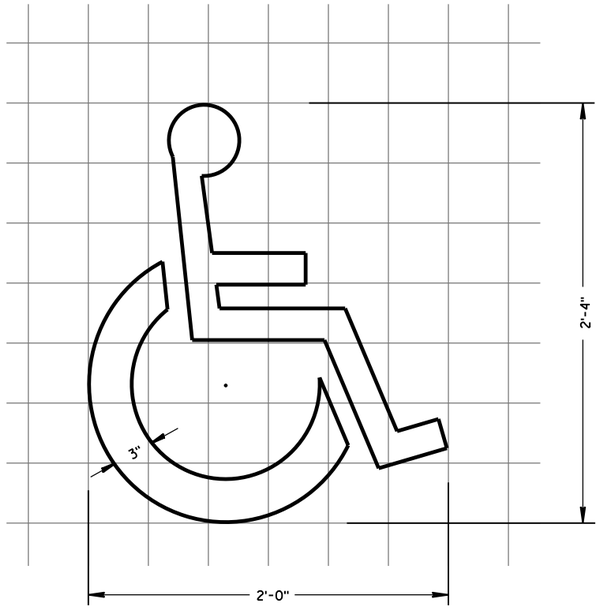
S.D.D. 15 C 3-3

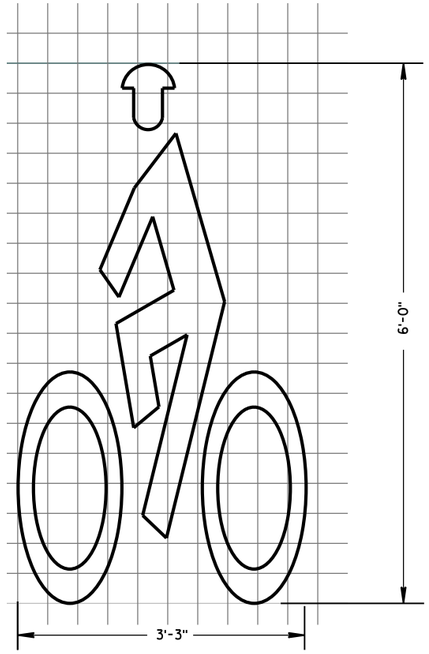
6

S.D.D. 15 C 3-3

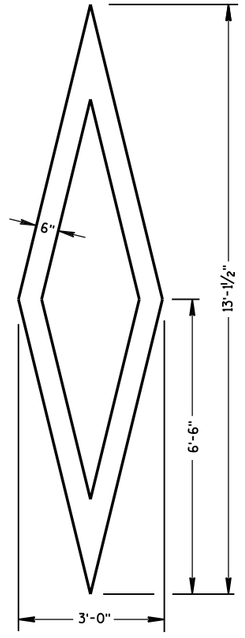
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.





BIKE CROSSING SYMBOL



PREFERENTIAL LANE SYMBOL

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S.D.D. 15 C 7-13a

S.D.D. 15 C 7-13a

PAVEMENT MARKING SYMBOLS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

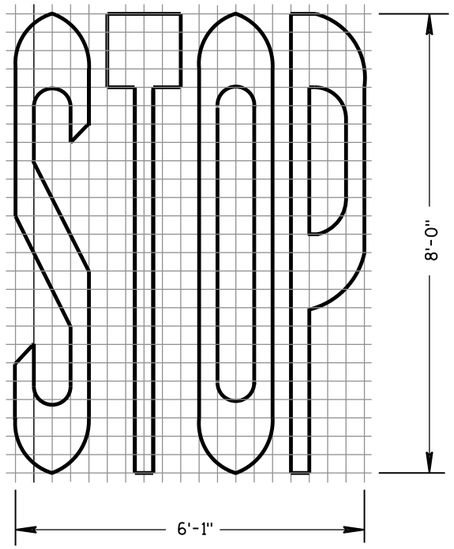
APPROVED

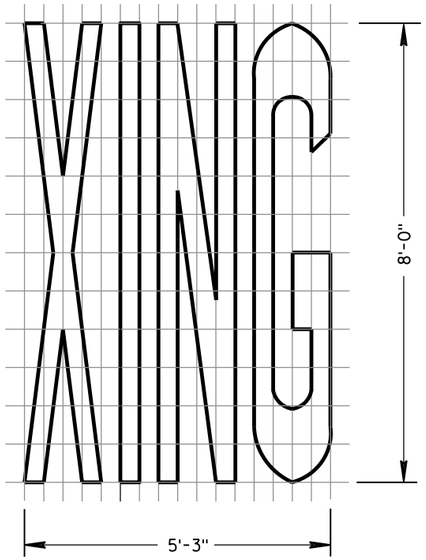
4/18/16	/S/ Matthew R. Rauch
DATE	STATE SIGNING AND MARK EER

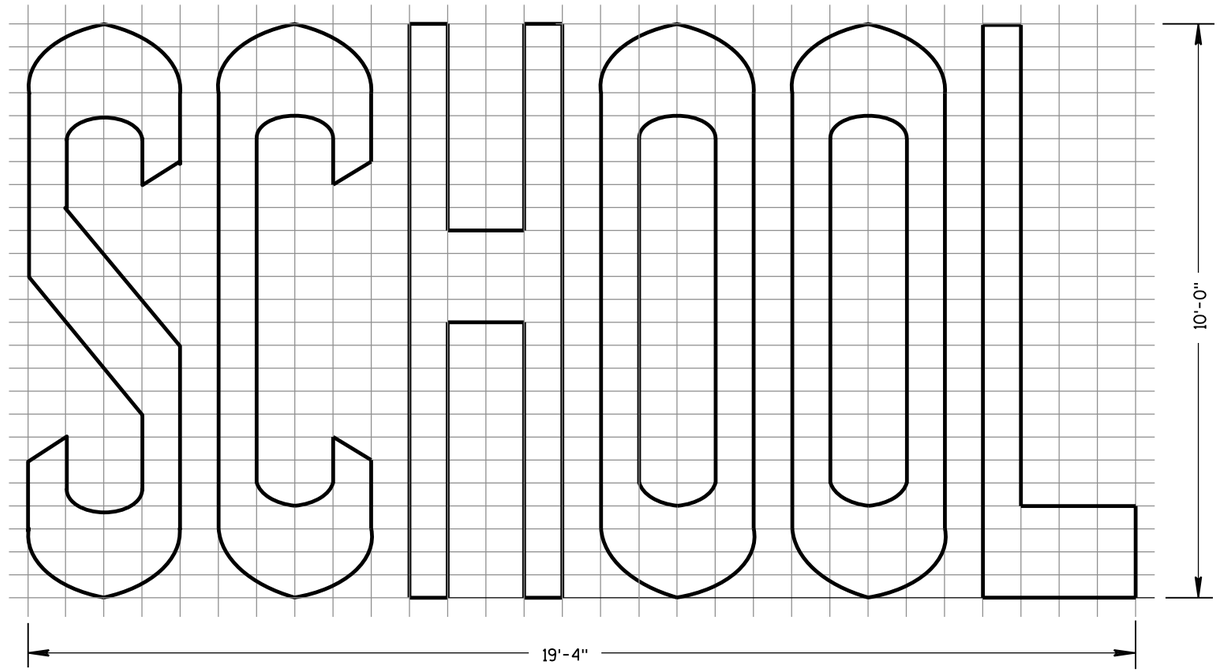
FHWA

GENERAL NOTES

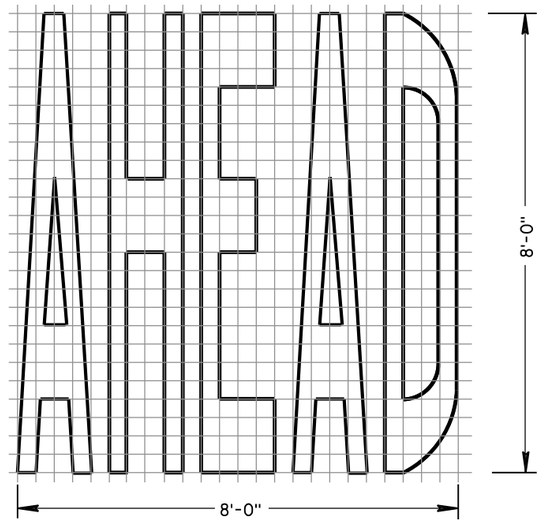
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

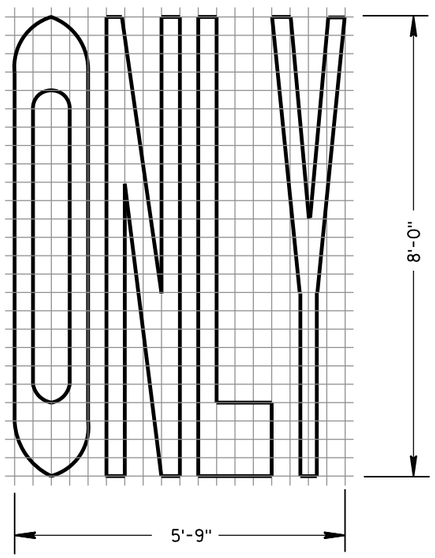


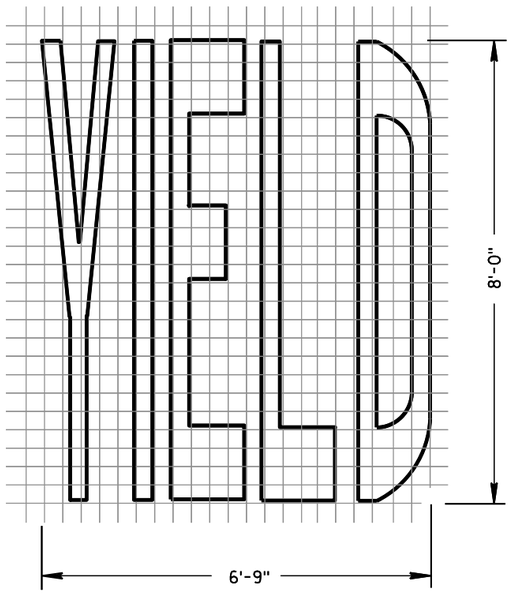


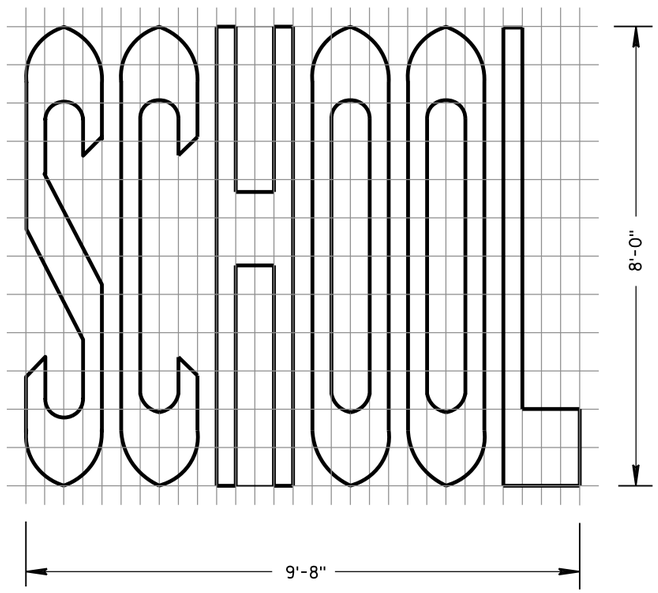


TWO-LANE









SINGLE-LANE

PAVEMENT MARKING WORDS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

4-18-16	/S/ Matthew R. Rauch	
DATE	STATE SIGNING AND MARK	EER

FHWA

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S.D.D. 15 C 7-13b

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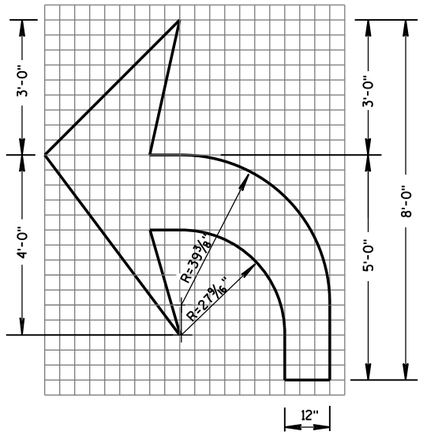
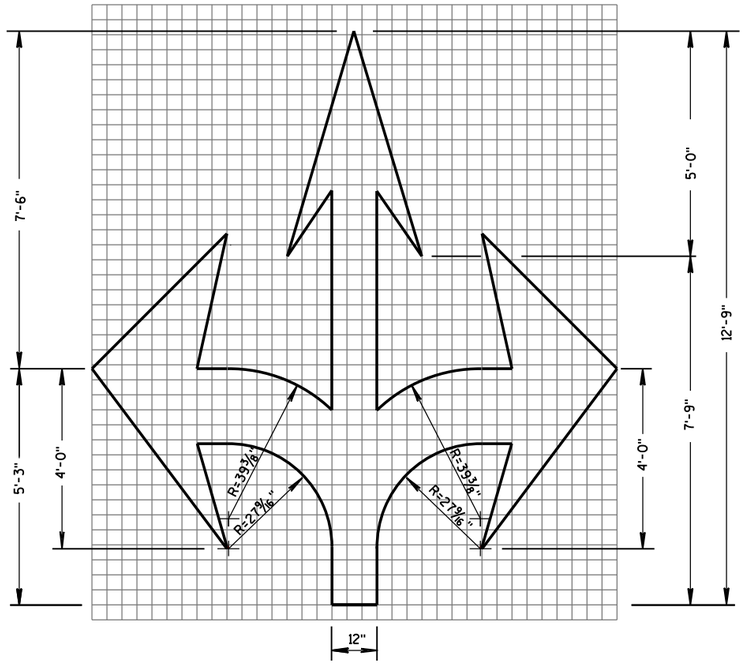
S.D.D. 15 C 7-13b

GENERAL NOTES

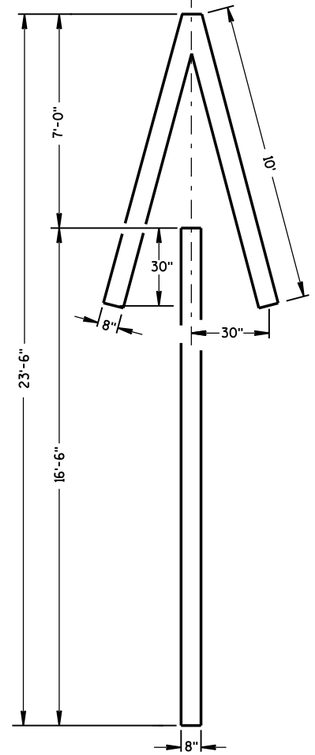
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



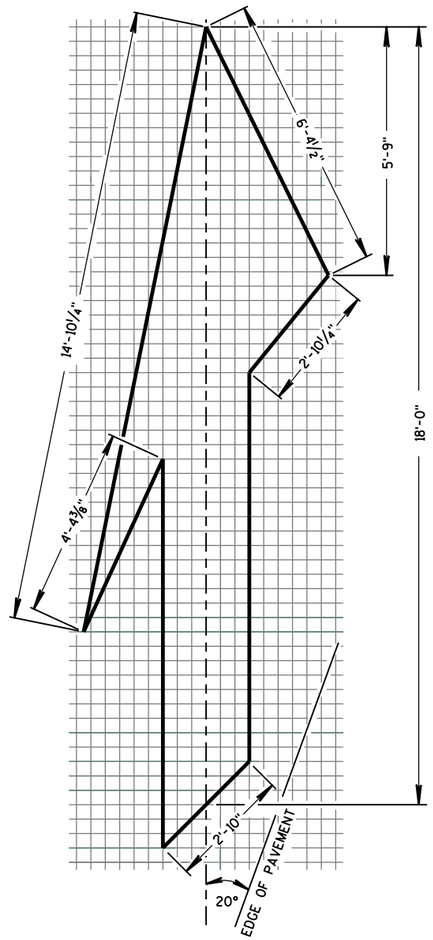
TYPE 2



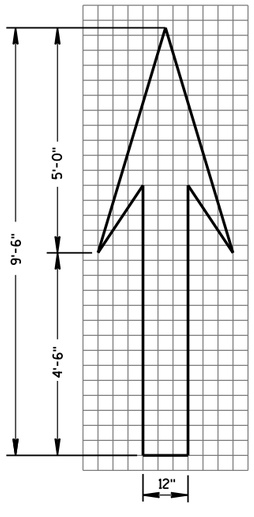
TYPE 6



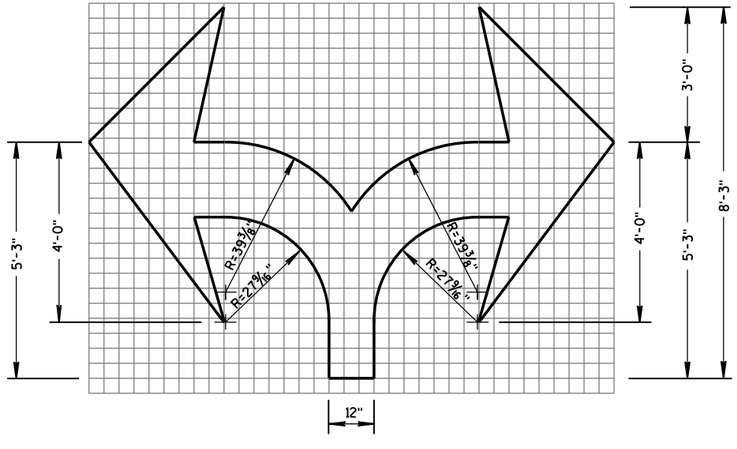
TYPE 4



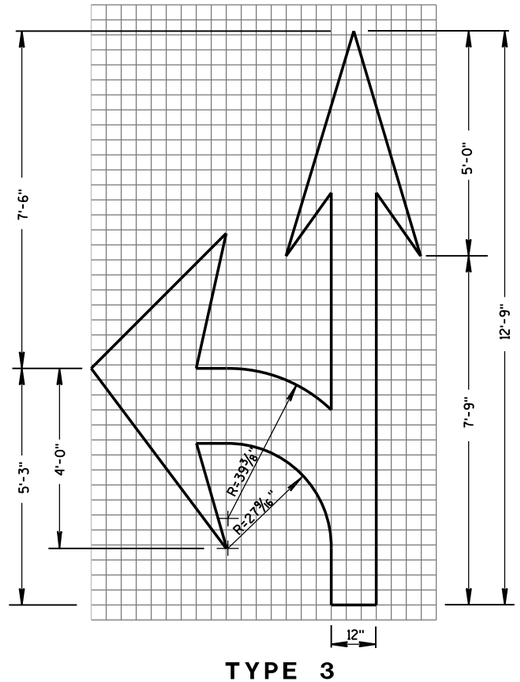
TYPE 5 LANE DROP ARROW



TYPE 1



TYPE 7



TYPE 3

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-18-16 DATE
/S/ Matthew R. Rauch
STATE SIGNING AND MAR
ENGINEER
FHWA

6

S.D.D. 15 C 7-13c

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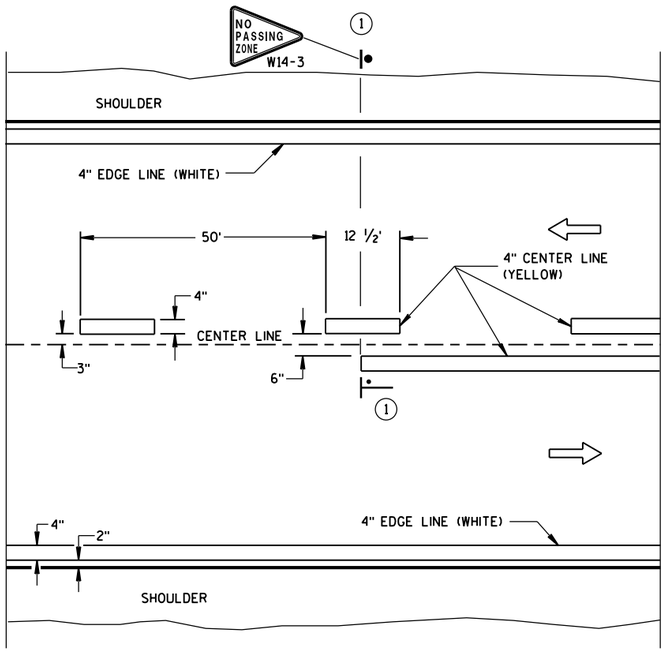
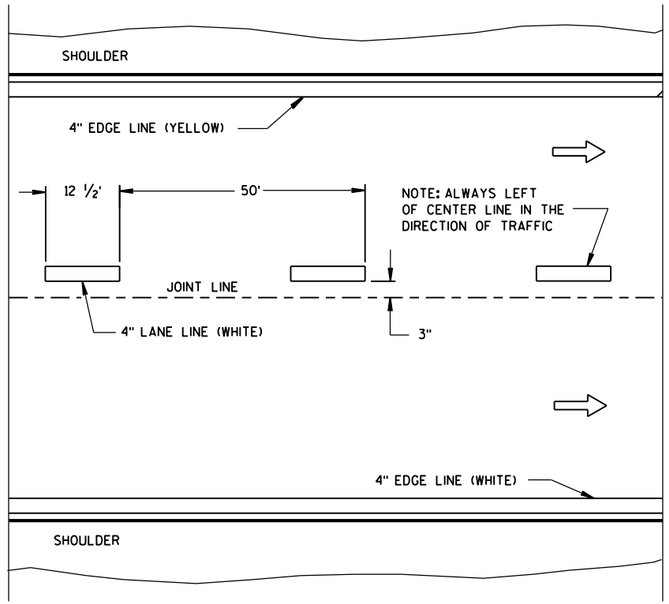
S.D.D. 15 C 7-13c



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) NO PASSING ZONE W14-3 SIGN SHALL BE LOCATED WITHIN 50 FEET OF THE "T" MARKING.

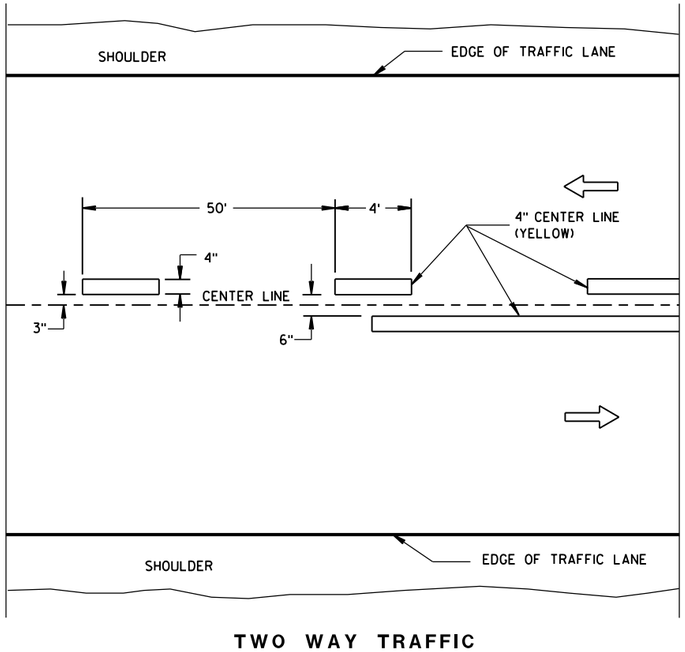
NOTE

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL

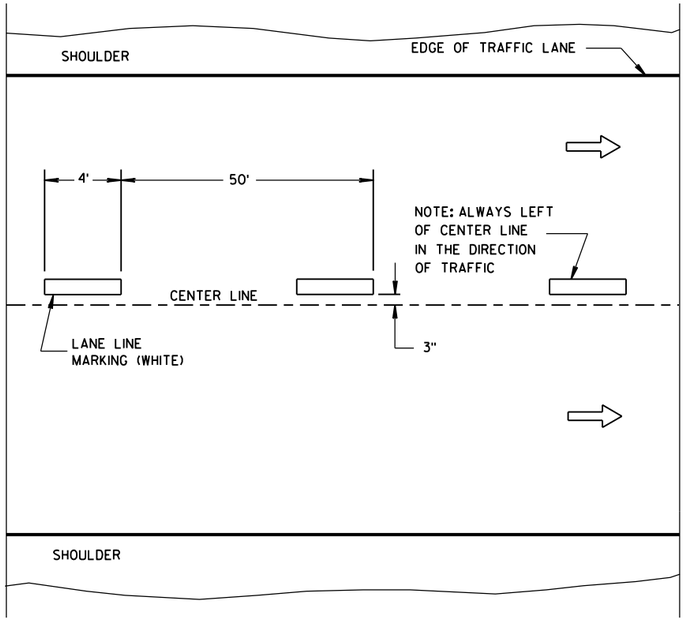
LEGEND

"T" MARKING

POST MOUNTED SIGN



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

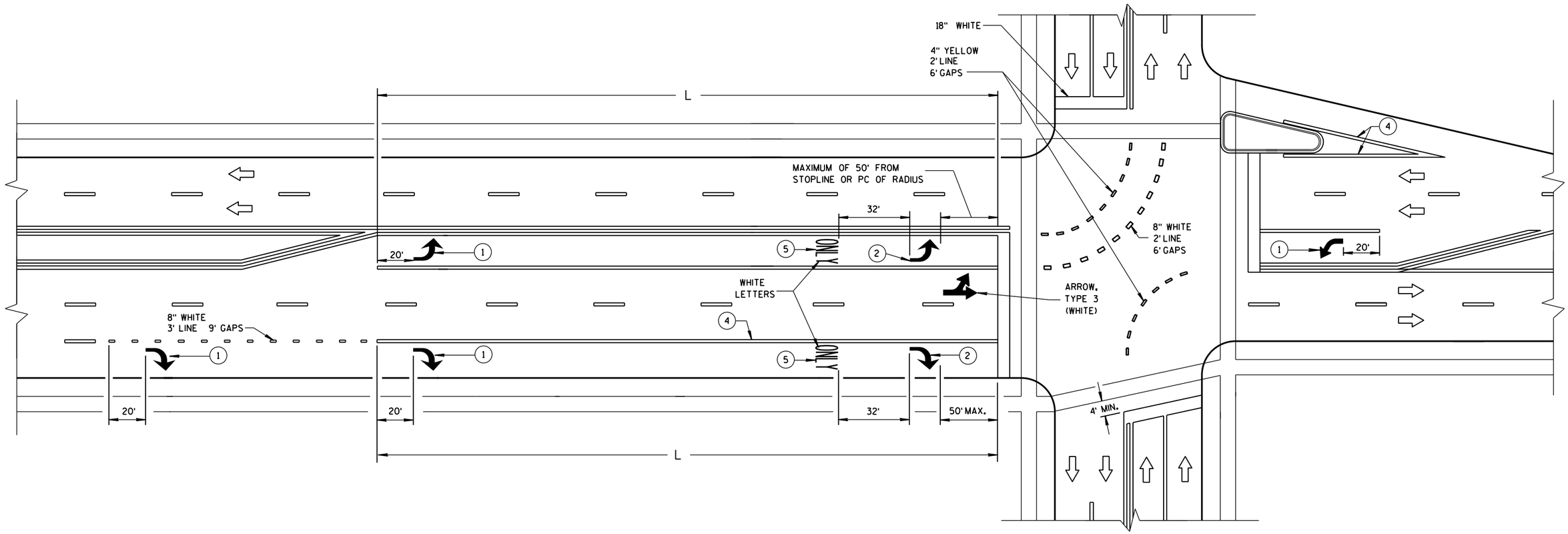
LONGITUDINAL MARKING (MAINLINE)

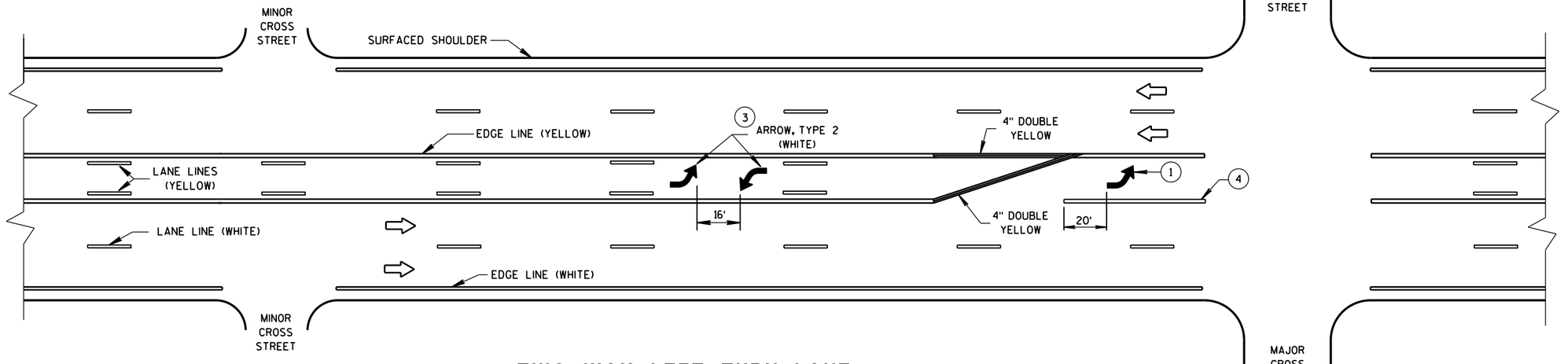
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2016 — DATE
/S/ Matthew R. Rauch — STATE SIGNING AND MARKING ENGINEER
FHWA

S.D.D. 15 C 8-17a

S.D.D. 15 C 8-17a





GENERAL NOTES

(1) REQUIRED ARROW, TYPE 2 (WHITE).

(2) REQUIRED ARROW, TYPE 2 (WHITE) WHEN L IS GREATER THAN 78 FEET AND LESS THAN OR EQUAL TO 166 FEET.

(3) A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.

(4) 8" WHITE

(5) REQUIRED WORD ONLY WHEN L IS GREATER THAN 166 FEET.

NOTE:
ARROW SYMBOL (⇨)
SHOWS DIRECTION OF TRAVEL

L = LENGTH OF TURN BAY

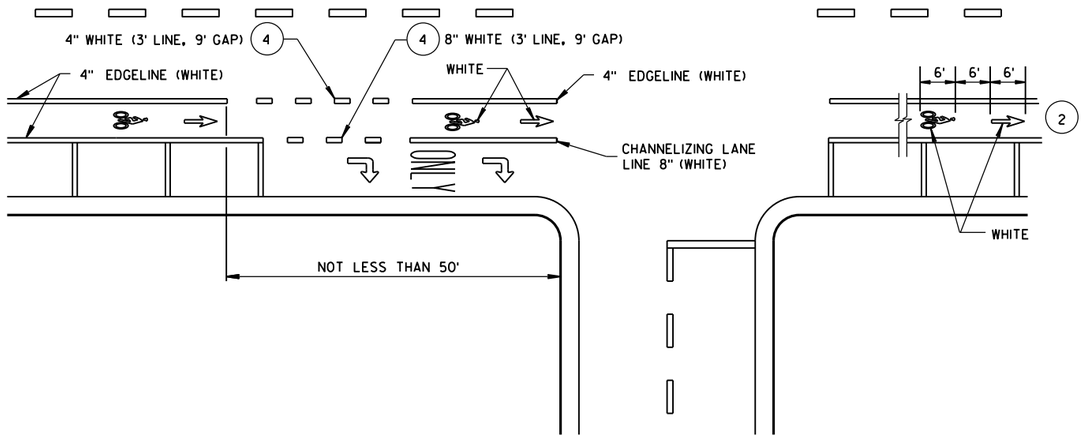
TWO WAY LEFT TURN LANE

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

6

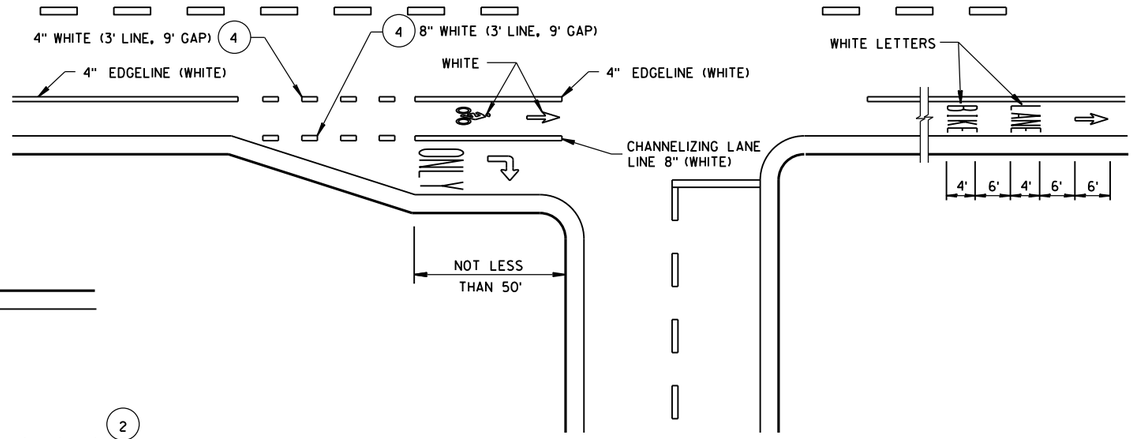
S.D.D. 15 C 8-17b



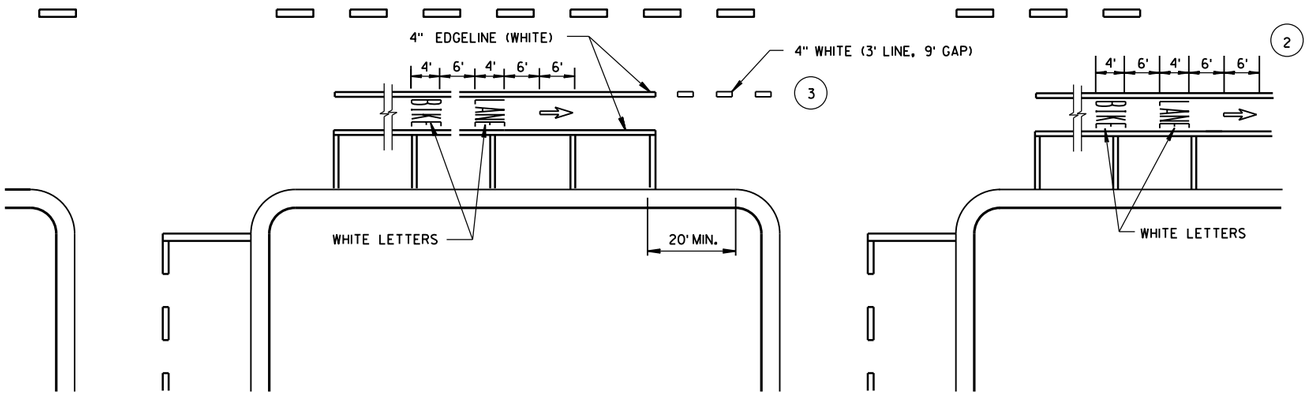
DESIGNATED BICYCLE LANE

GENERAL NOTES

1. DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
2. THE SERIES OF PAVEMENT MARKING SYMBOLS SHALL BE REPEATED AFTER INTERSECTIONS AND SPACED A MAXIMUM OF 250'. NO PAVEMENT MARKING WILL TAKE PLACE IN THE CROSSWALK.
3. DOTTED LINES SHOULD BE USED 50' TO 200' IN ADVANCE OF AN INTERSECTION WHERE THERE IS NO RIGHT TURN ONLY LANE AND THERE IS HEAVY RIGHT TURN TRAFFIC OR THERE IS A NEAR-SIDE BUS STOP. AT OTHER INTERSECTIONS WHERE RIGHT TURN TRAFFIC IS LIGHT TO MODERATE, A SOLID LINE CAN BE USED UP TO THE INTERSECTION.
4. WHEN SPECIFIED IN THE CONTRACT.



DESIGNATED BICYCLE LANE
NO PARKING, RIGHT TURN LANE



DESIGNATED BICYCLE LANE
WITH PARKING, NO RIGHT TURN LANE

URBAN BICYCLE LANE MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

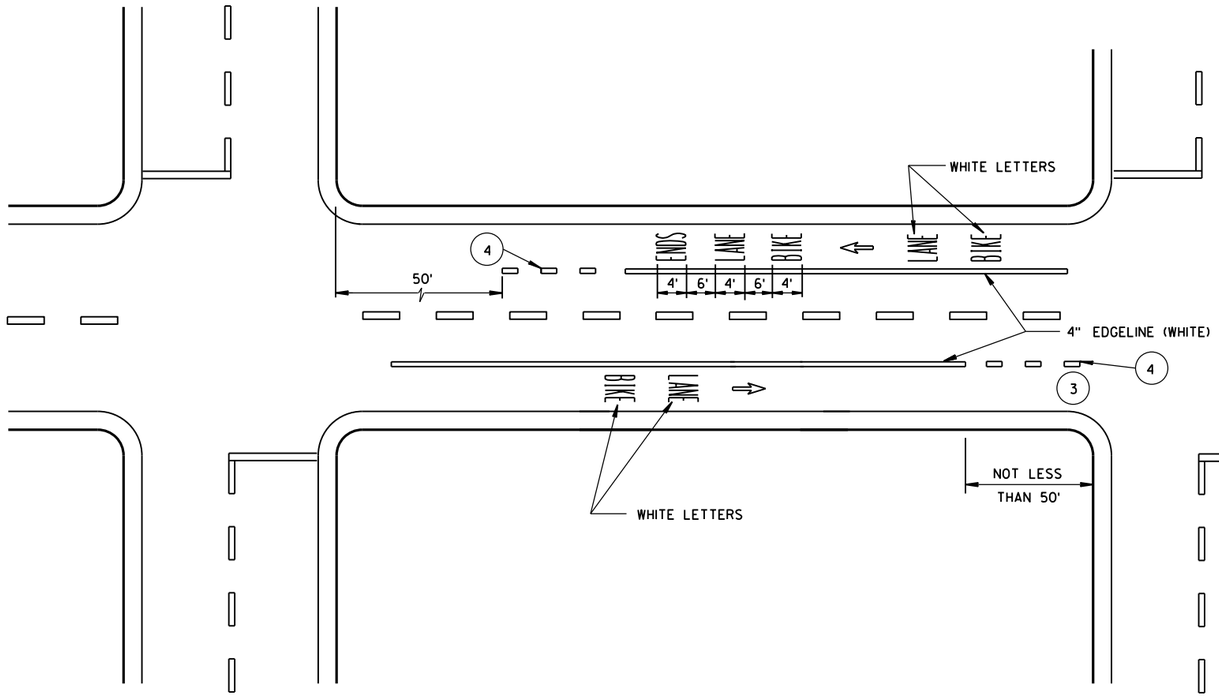
APPROVED
4/30/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC E
FHWA

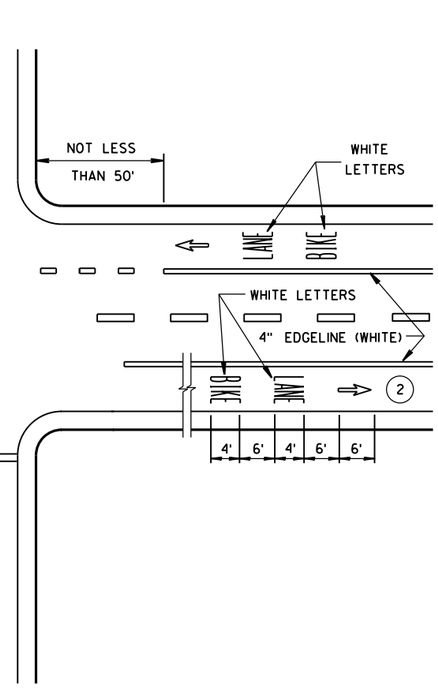
S.D.D. 15 C 29-3c

S.D.D. 15 C 29-3c

GENERAL NOTES

1. DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
2. THE SERIES OF PAVEMENT MARKING SYMBOLS SHALL BE REPEATED AFTER INTERSECTIONS AND SPACED A MAXIMUM OF 250'. NO PAVEMENT MARKING WILL TAKE PLACE IN THE CROSSWALK.
3. DOTTED LINES SHOULD BE USED 50' TO 200' IN ADVANCE OF AN INTERSECTION WHERE THERE IS NO RIGHT TURN ONLY LANE AND THERE IS HEAVY RIGHT TURN TRAFFIC OR THERE IS A NEAR-SIDE BUS STOP. AT OTHER INTERSECTIONS WHERE RIGHT TURN TRAFFIC IS LIGHT TO MODERATE, A SOLID LINE CAN BE USED UP TO THE INTERSECTION.
4. 3' LINE, 9' GAP - 4" WIDE, WHITE.





DESIGNATED BICYCLE LANE
NO PARKING

URBAN BICYCLE LANE MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/30/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC EN
FHWA

6

S.D.D. 15 C 29-3d

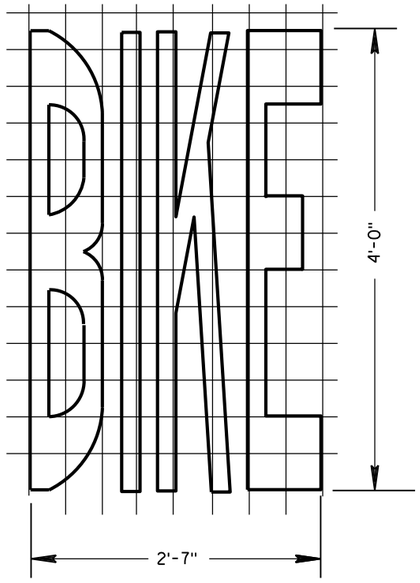
6

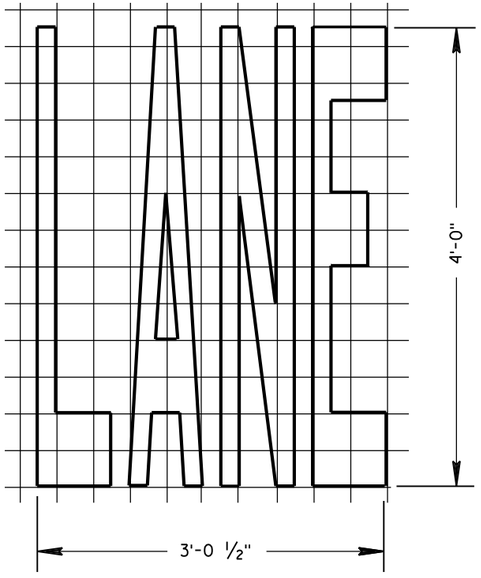
S.D.D. 15 C 29-3d

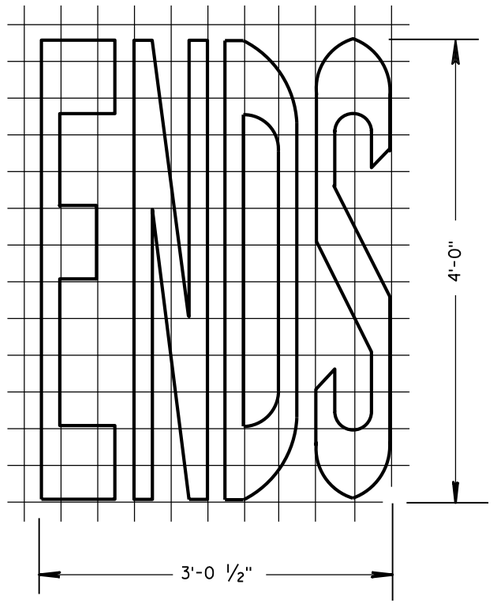
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

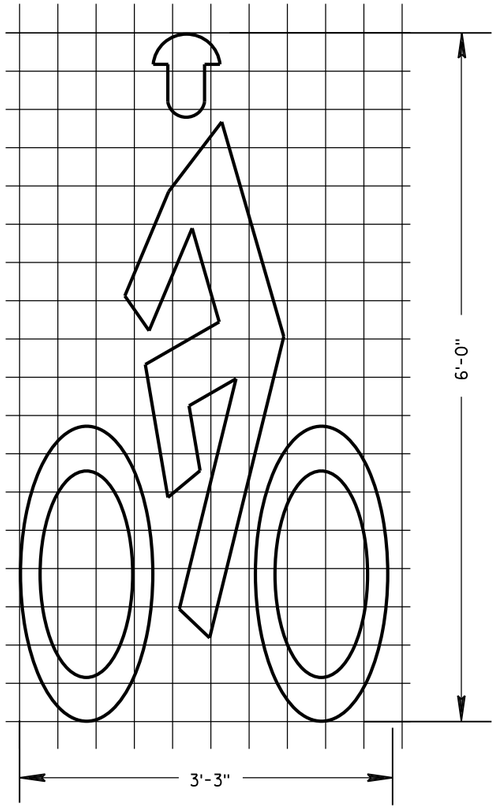
ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



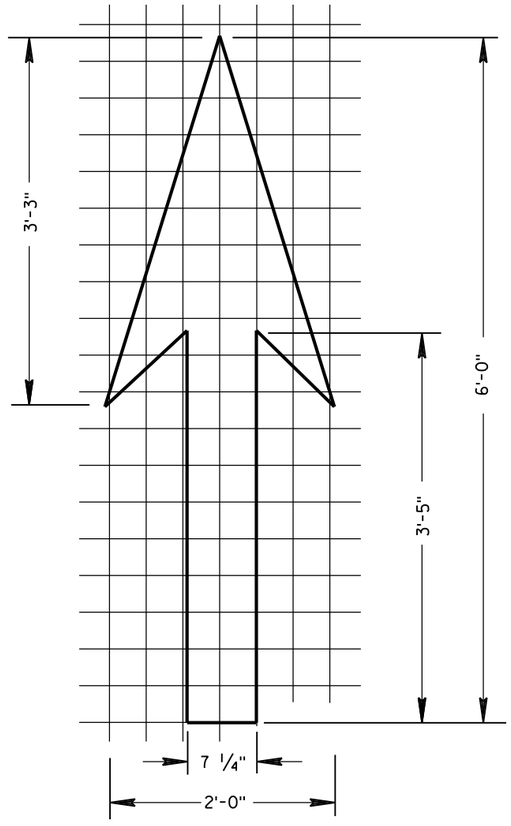




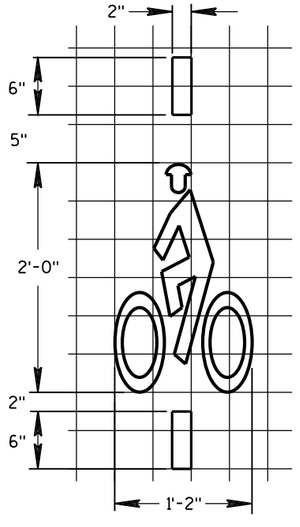
BIKE LANE WORDS



BIKE LANE SYMBOL



BIKE LANE ARROW



BICYCLE DETECTOR PAVEMENT MARKING

PAVEMENT MARKING FOR BIKE LANES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-30-2013
DATE

/S/ Travis Feltes
STATE TRAFFIC EN

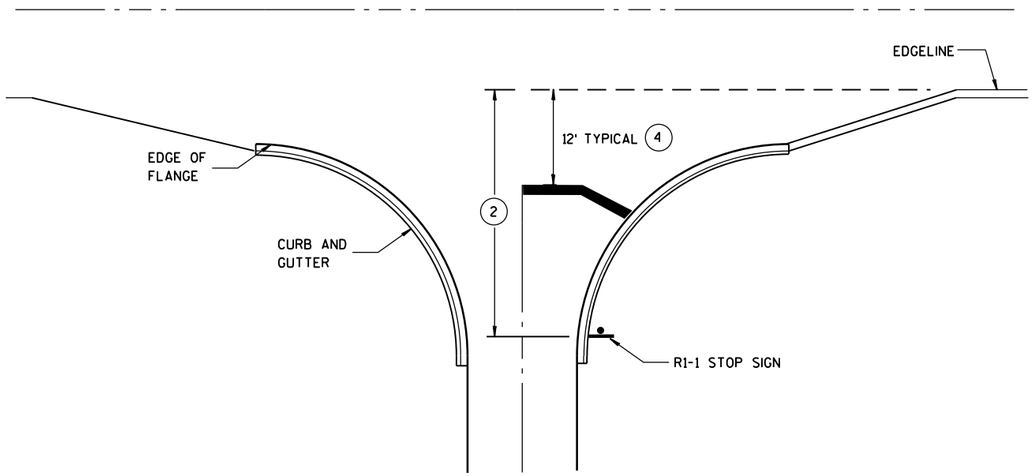
FHWA

6

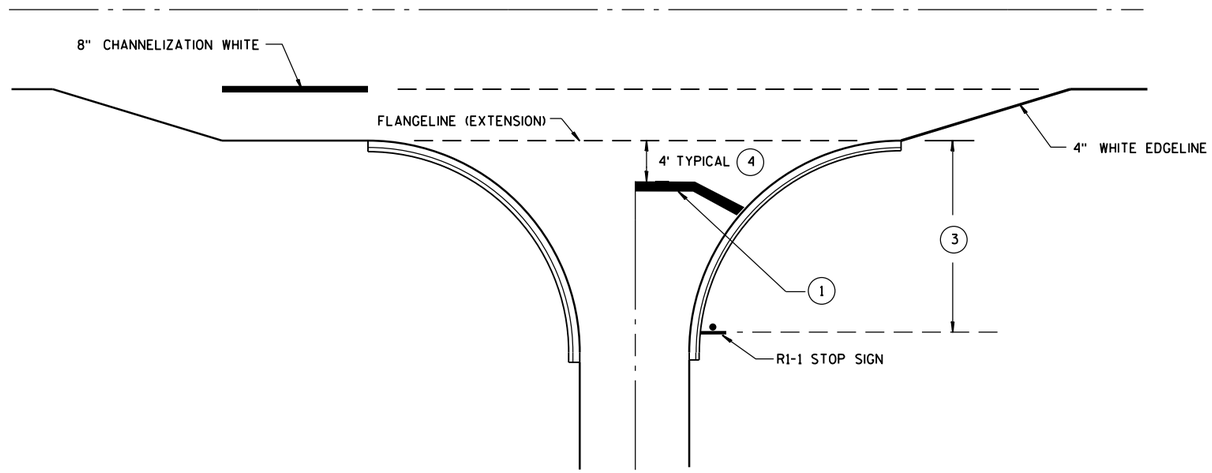
S.D.D. 15 C 29-3e

6

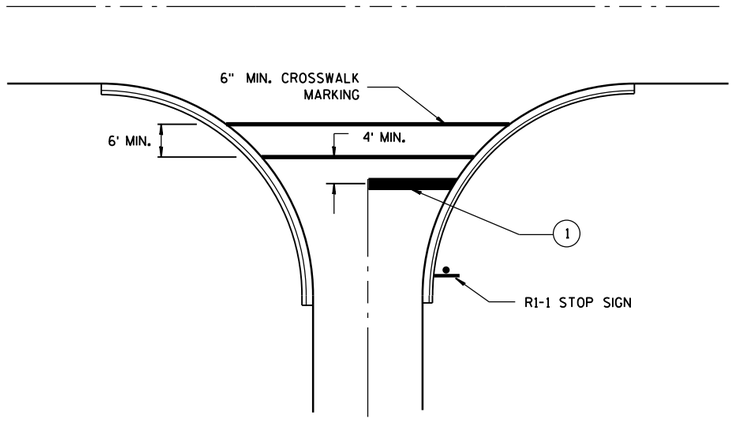
S.D.D. 15 C 29-3e



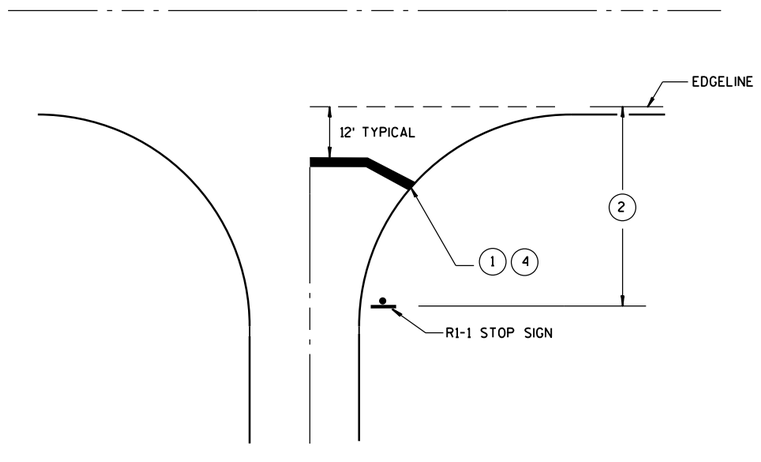
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
3. IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
4. MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

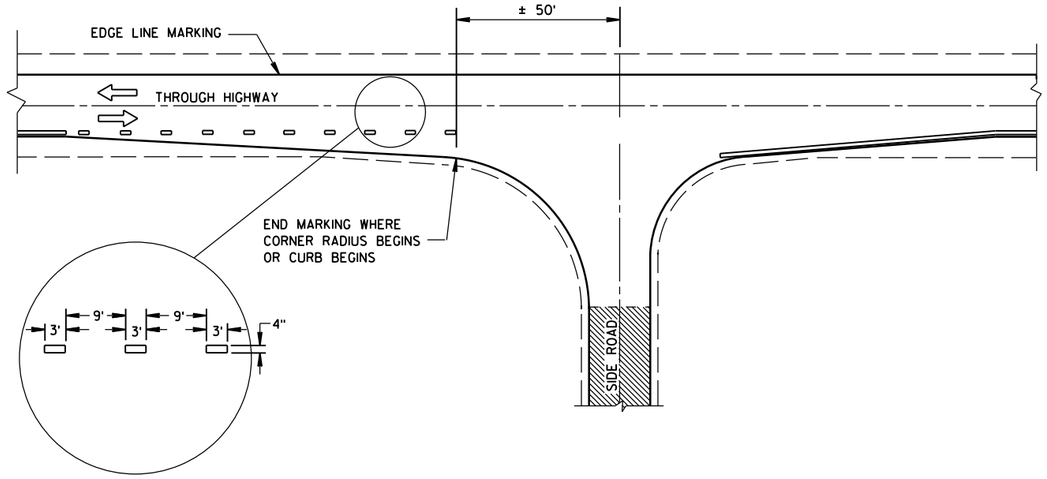
4-18-2016
DATE

/S/ Matthew R. Rauch
STATE SIGNING AND MAR[illegible]NEER

FHWA

6

S.D.D. 15 C 33-2



MINOR INTERSECTION WITHOUT CURBS

GENERAL NOTES

EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.

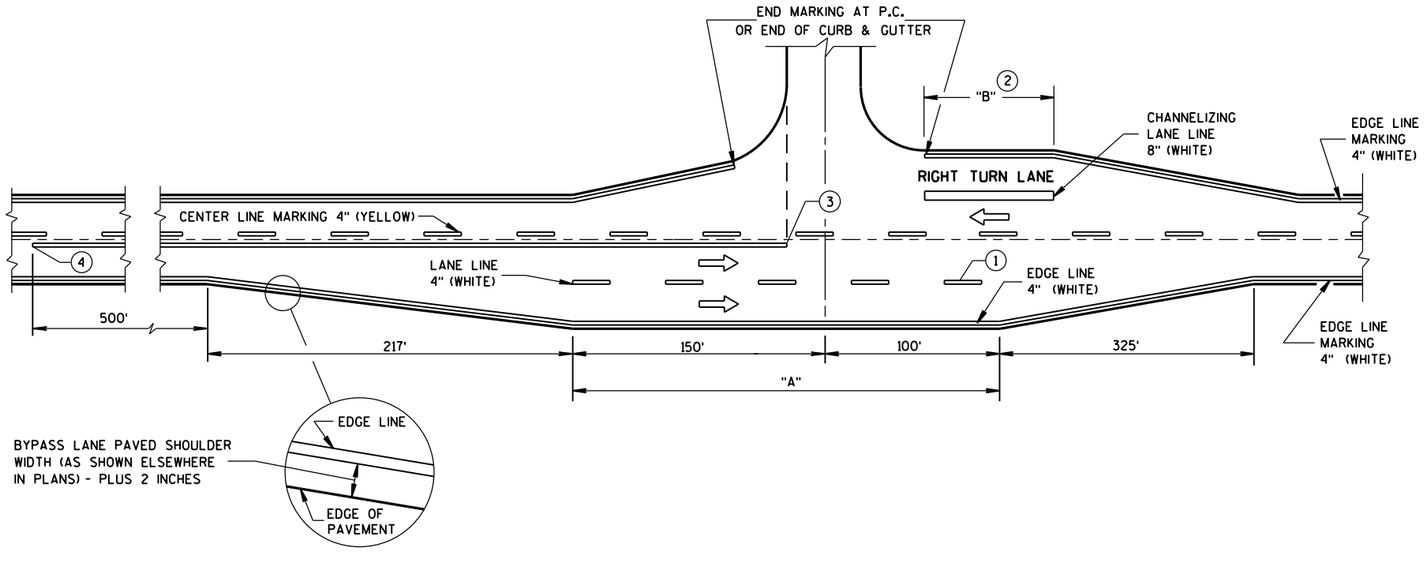
(1) WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.

(2) WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.

(3) BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.

(4) BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

ARROW SYMBOL (⇨) SHOWS DIRECTION OF TRAVEL



MAJOR INTERSECTIONS

(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)

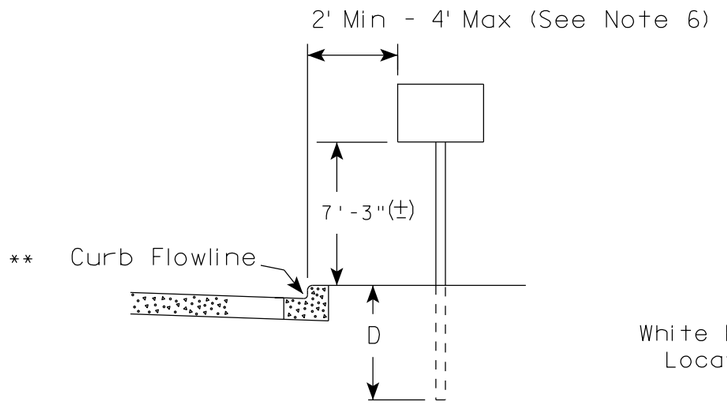
PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

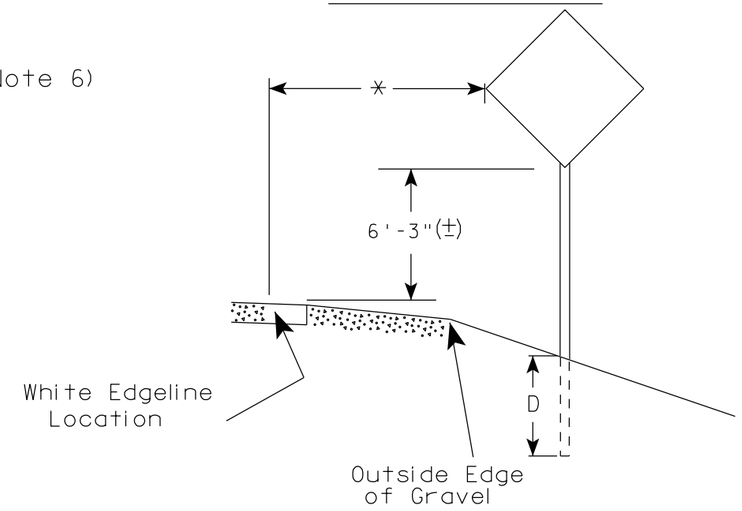
6

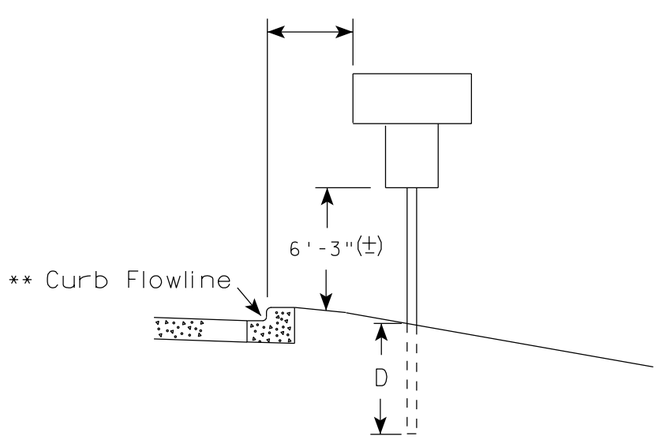
S.D.D. 15 C 35-1a

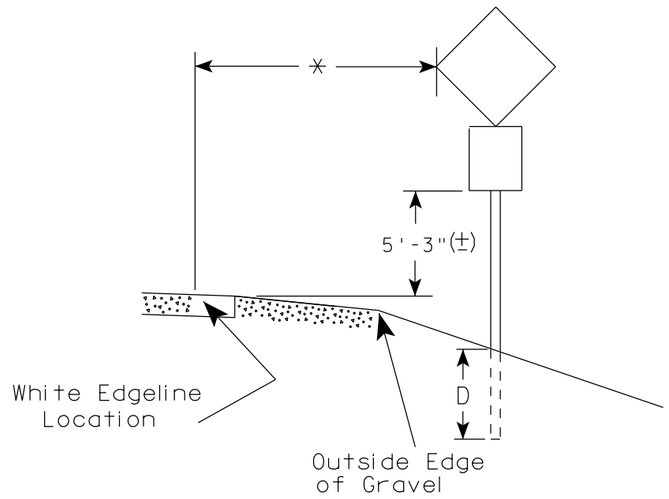
URBAN AREA



RURAL AREA (See Note 2)







GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

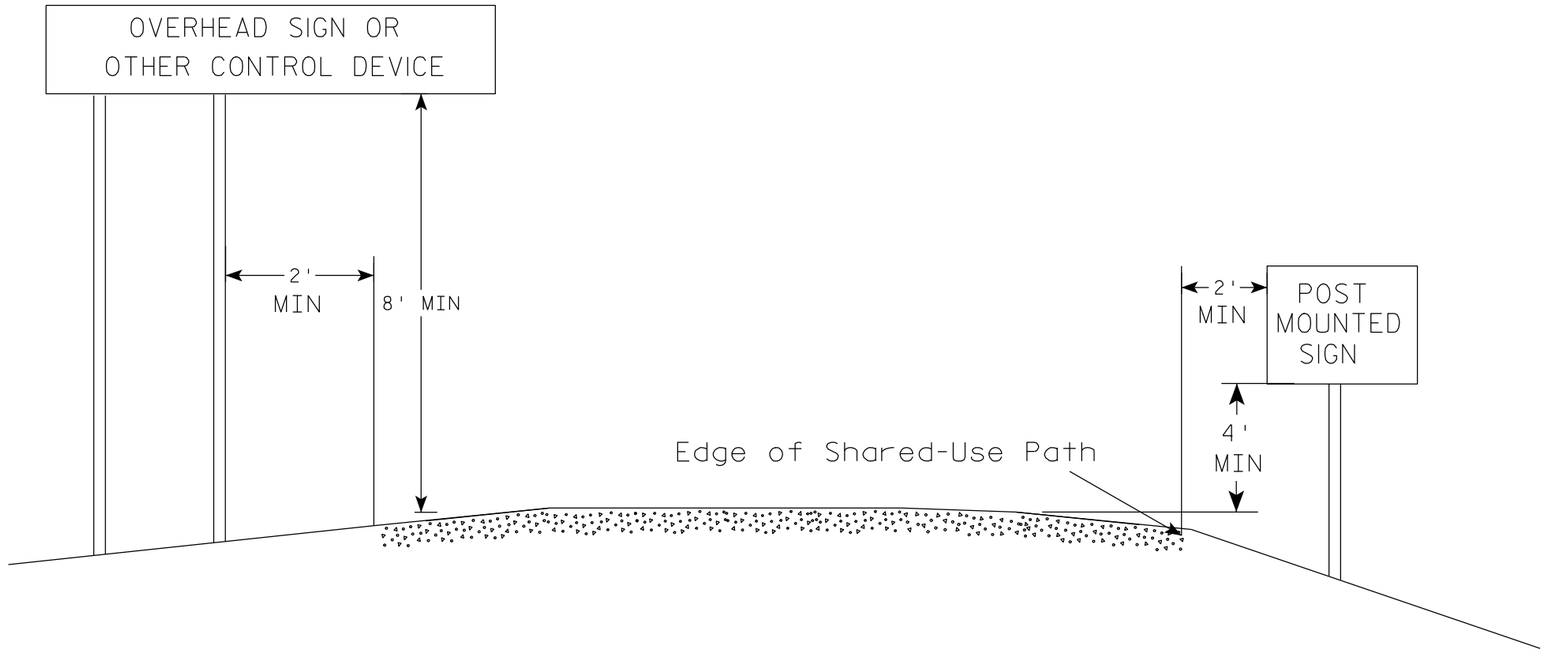
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-3.20

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

GENERAL NOTES

1. Signs wider than 4 feet or larger than 20 sq. ft. shall be mounted on multiple posts. Refer to plate A4-4.
2. Offset distance shall be consistent with existing signs or consistent throughout length of project.



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON MULTI USE PATHS

WISCONSIN DEPT OF TRANSPORTATION

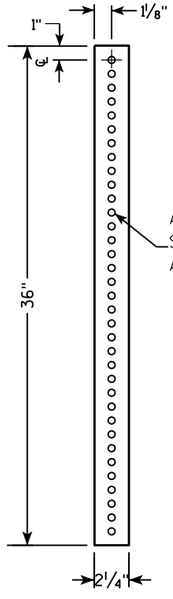
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/5/2012 PLATE NO. A4-3S.1

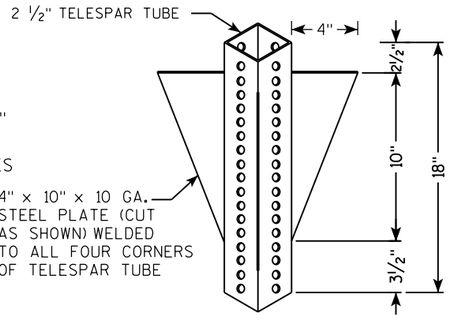
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 107	E

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

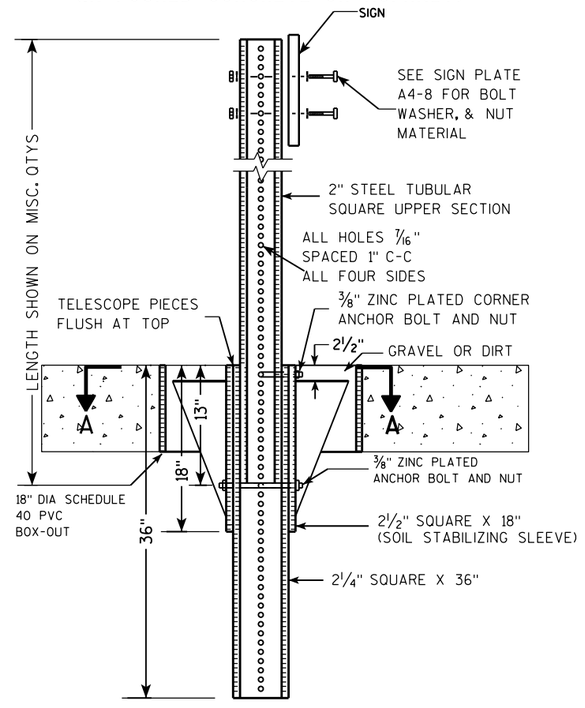
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



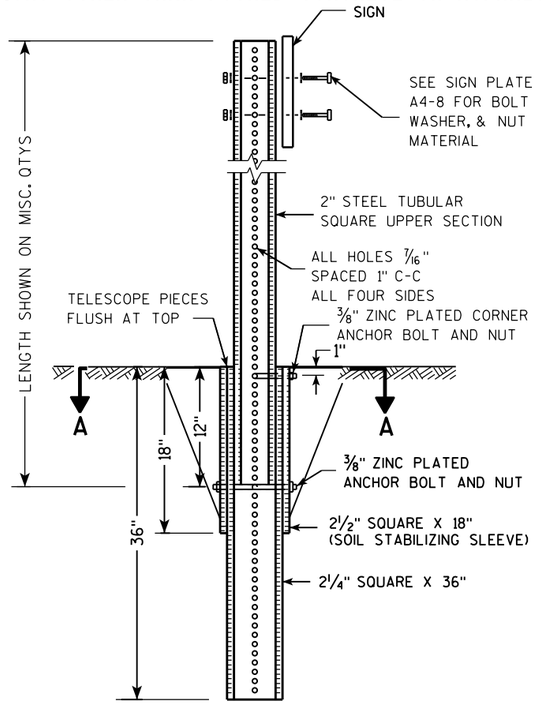
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

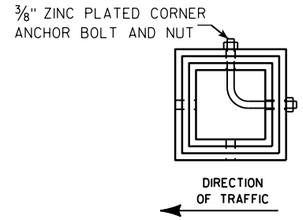


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

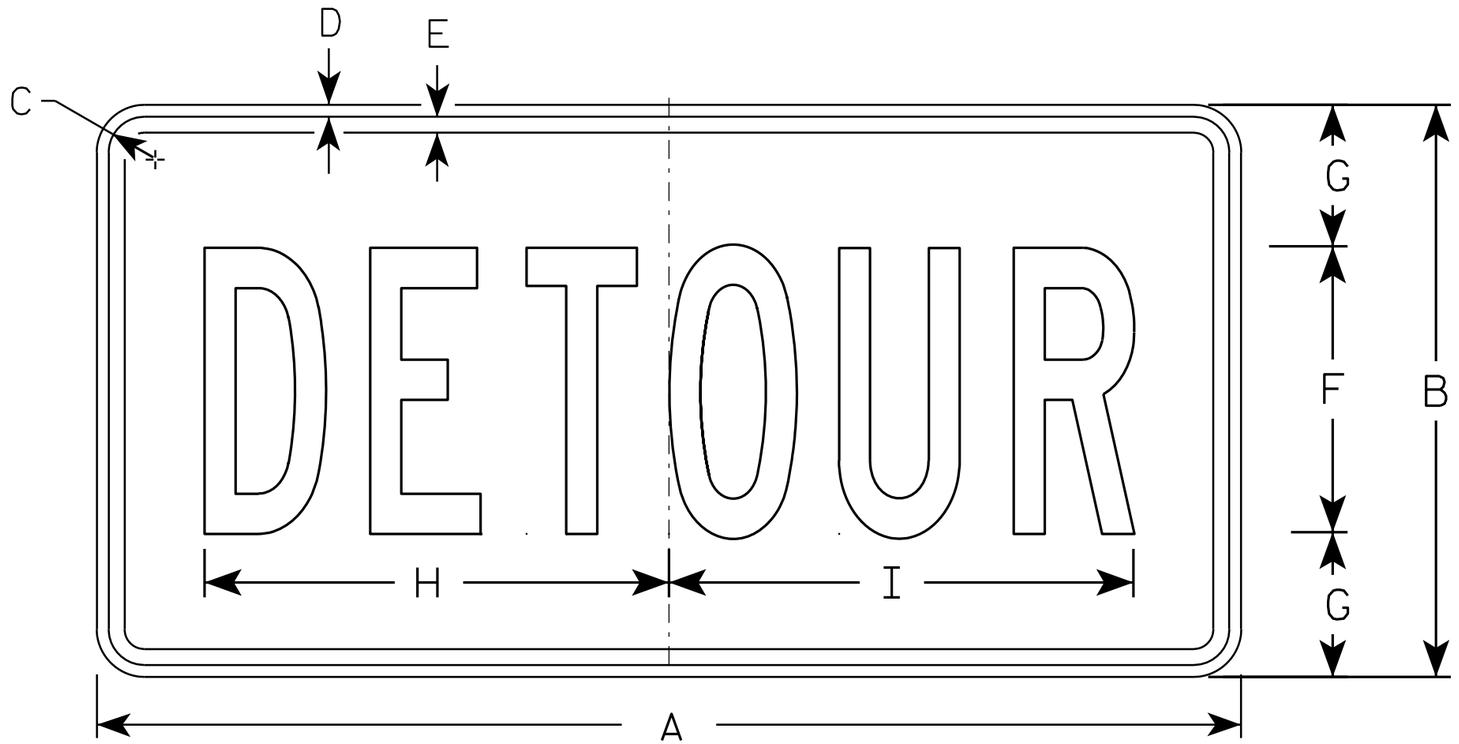
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000
WISDOT/CADDS SHEET 42



M4-8

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

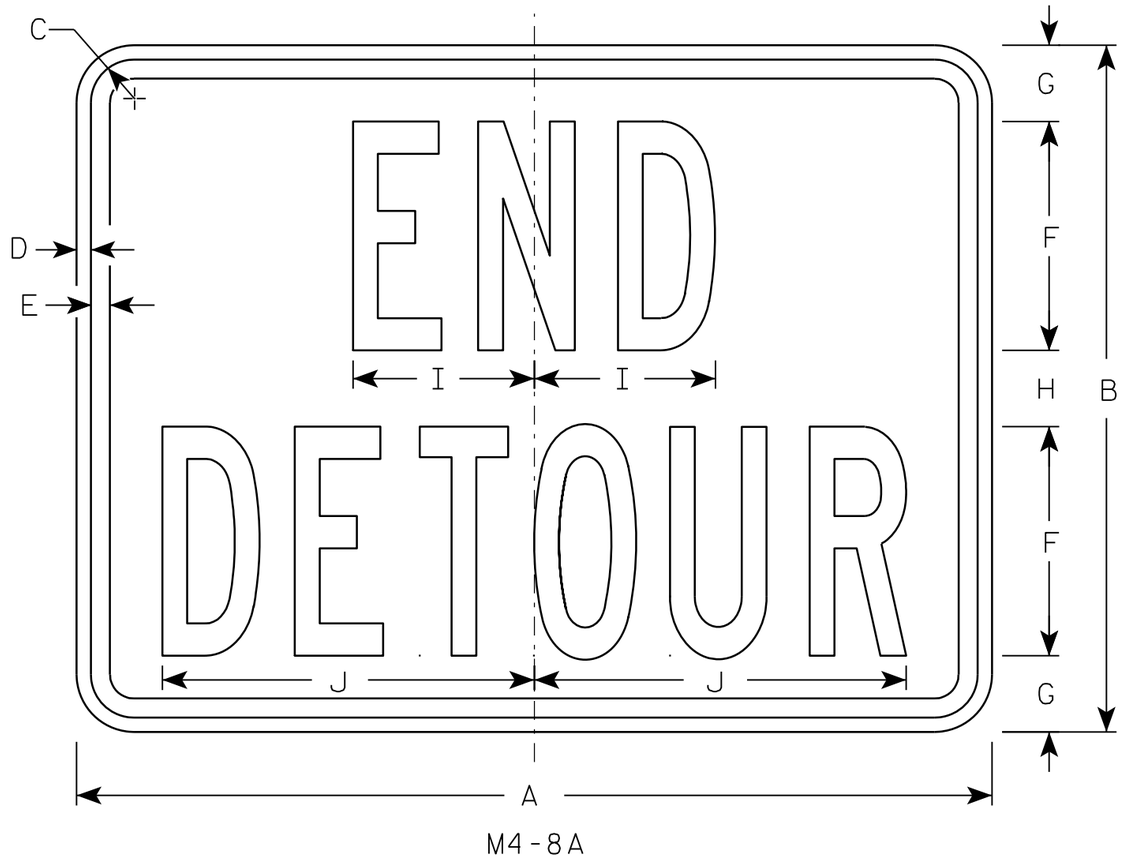
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 109	E



M4-8A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

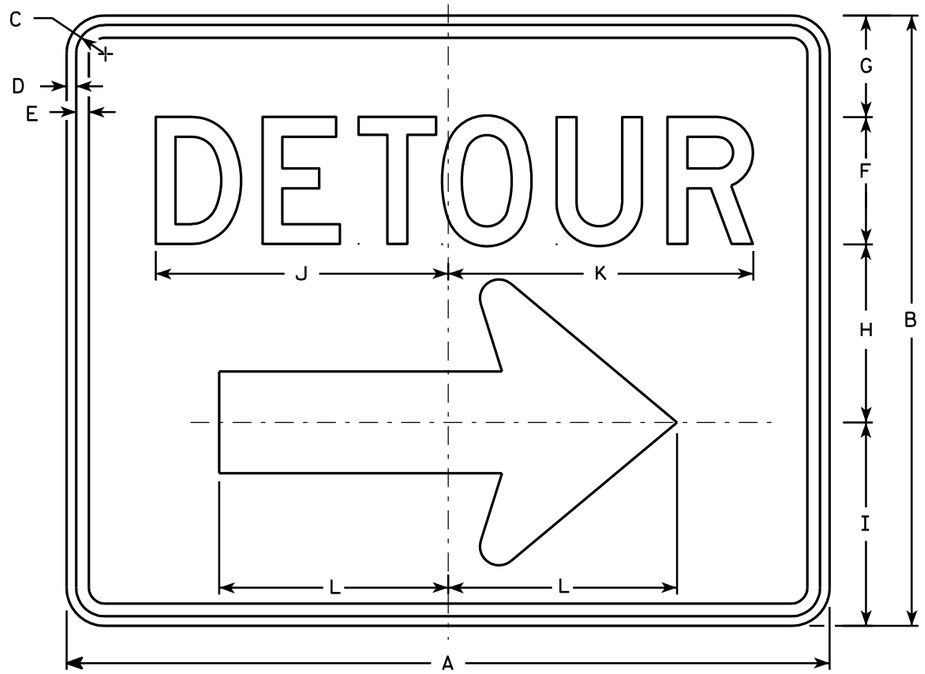
STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

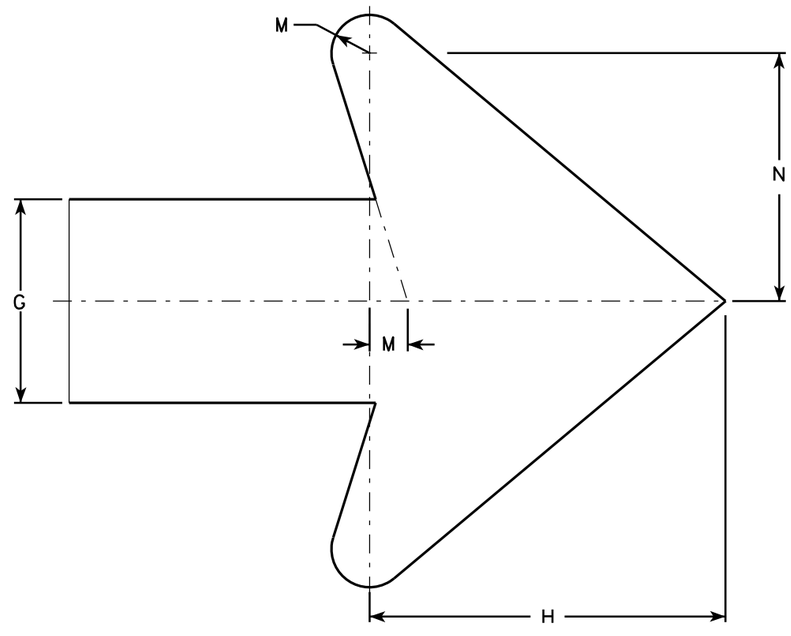
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 110	E



M4-9R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

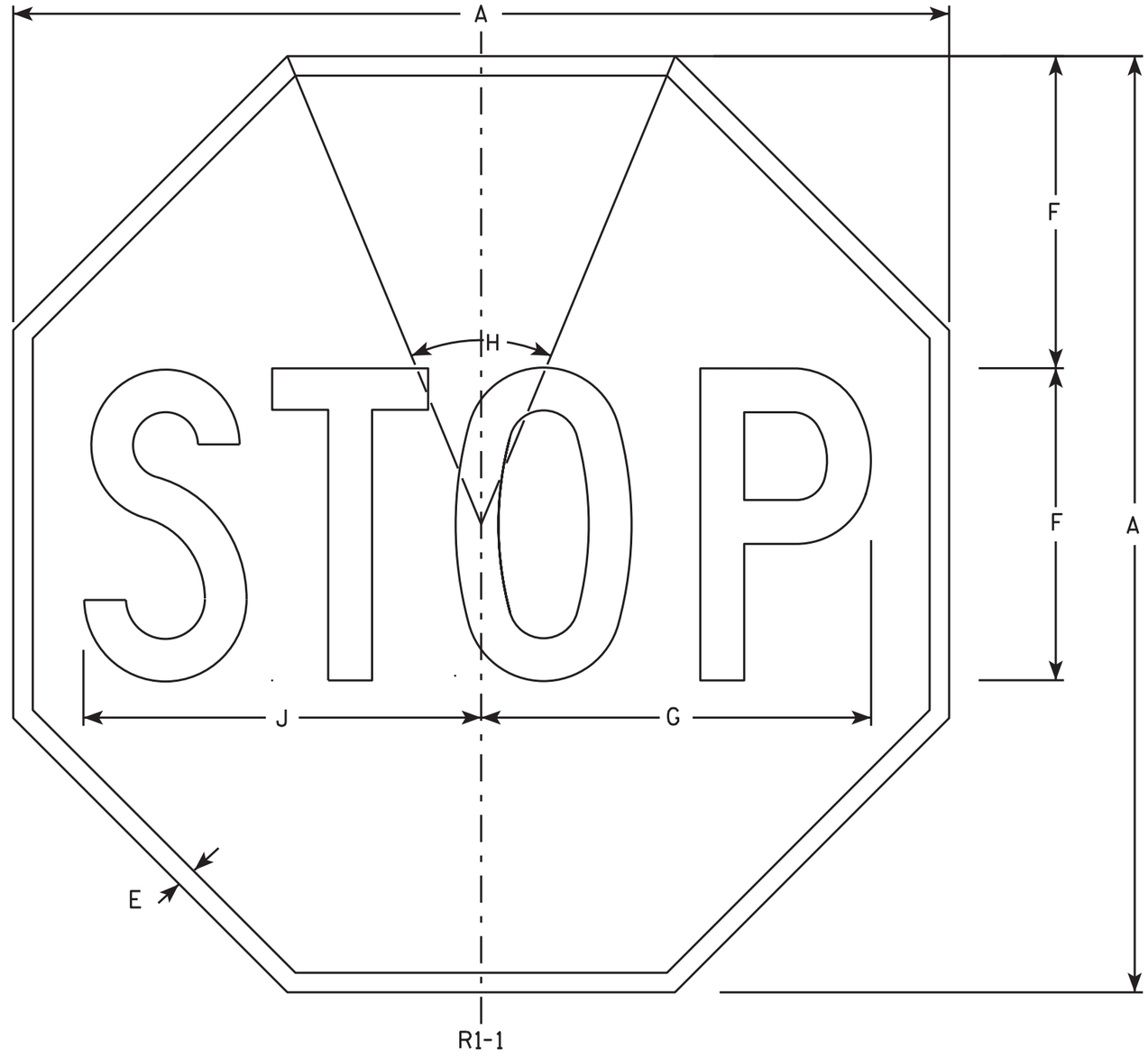
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 111	E



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

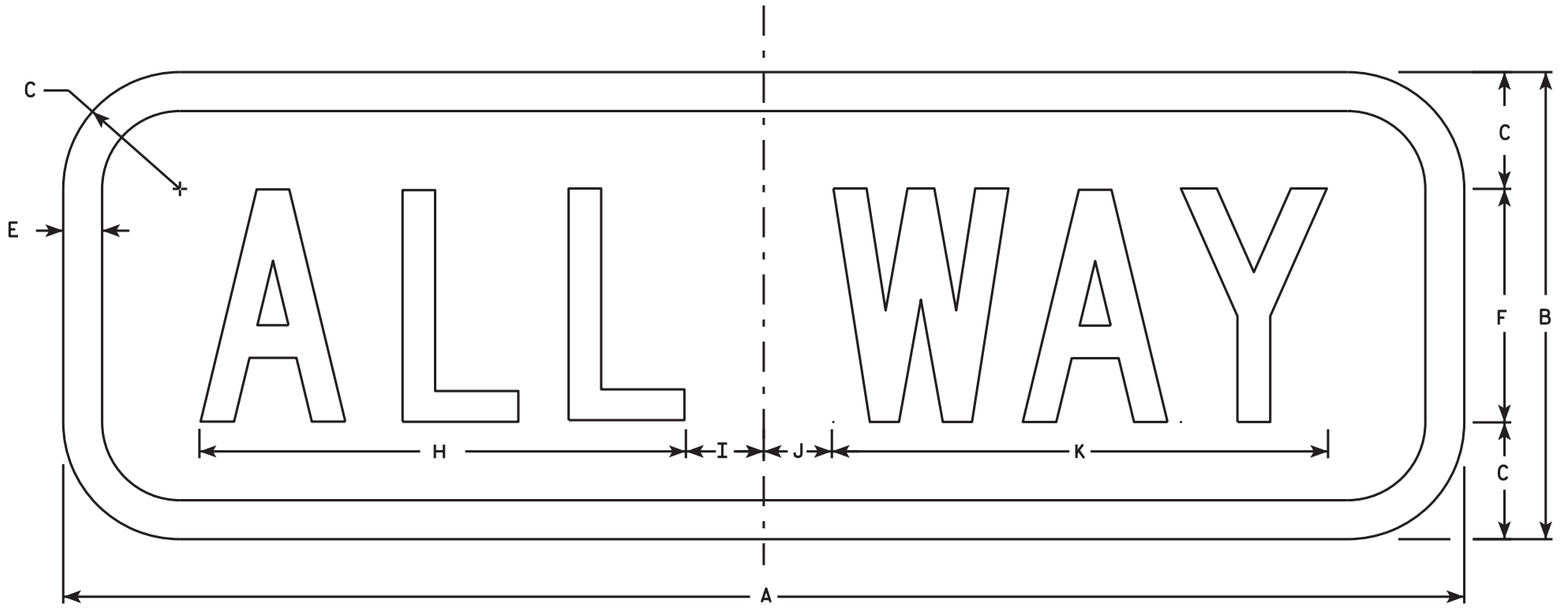
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.12

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



R1-3P

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																0.75
2S	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																1.5
2M	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
3	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
4	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5
5	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5

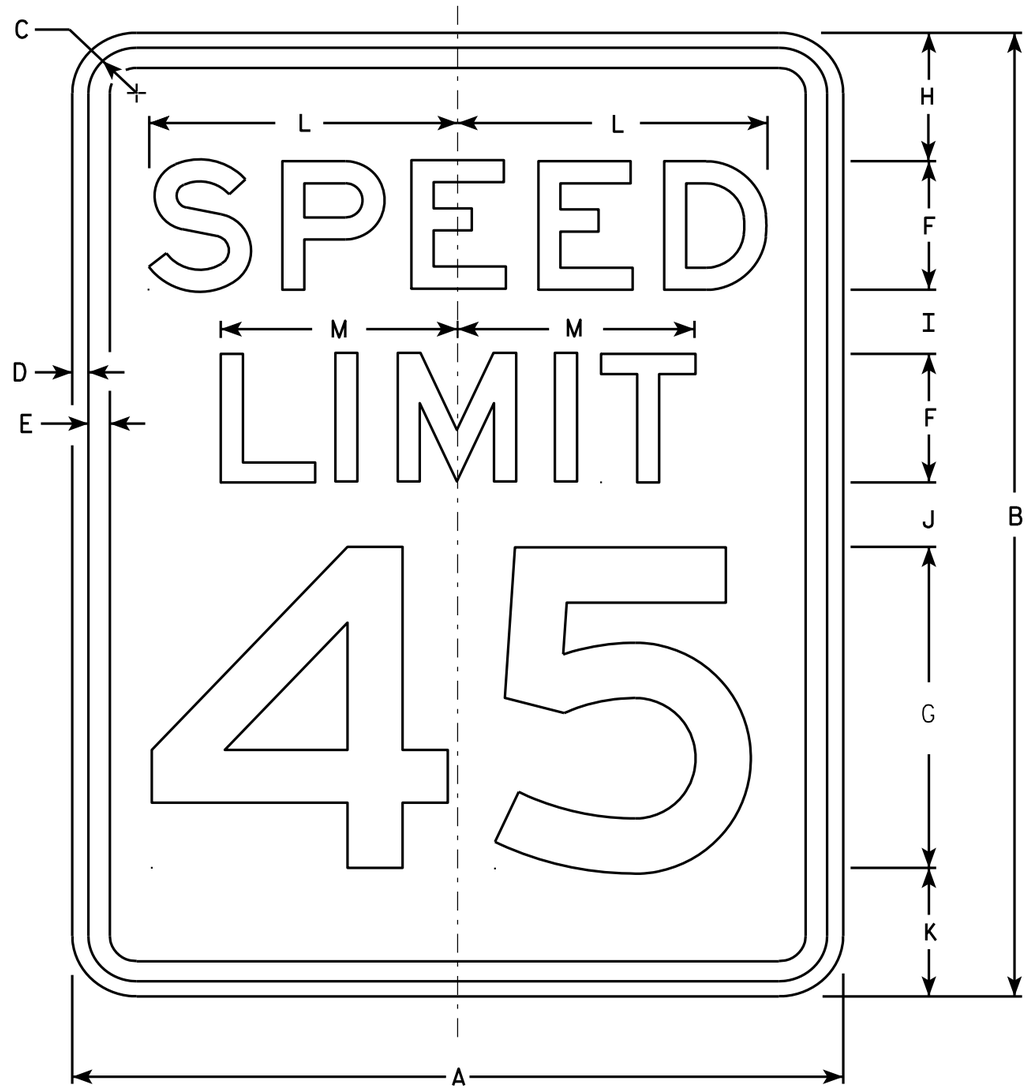
STANDARD SIGN
R1-3P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/24/15 PLATE NO. R1-3P.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

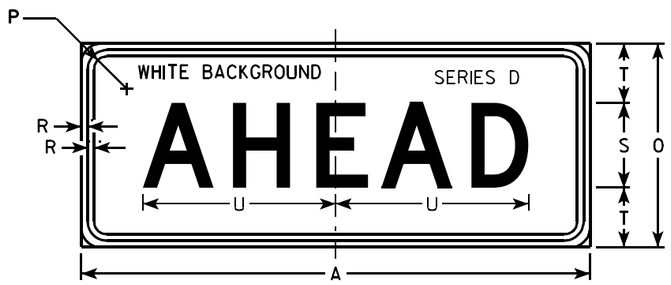
DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 114 | E

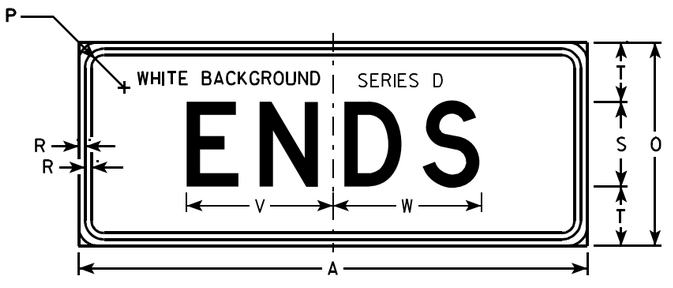
R3-17

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - AS SHOWN
   Message - BLACK
3. Message Series - C or as noted on the Signs.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R3-17ap



R3-17bp

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	R3-17 Area sq. ft.	R3-17ap Area sq. ft.	R3-17bp Area sq. ft.
1																													
2S	30	24	1 1/8	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/8	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
2M	30	24	1 1/8	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/8	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
3																													
4																													
5																													

STANDARD SIGN
R3-17 & R3-17a&bp

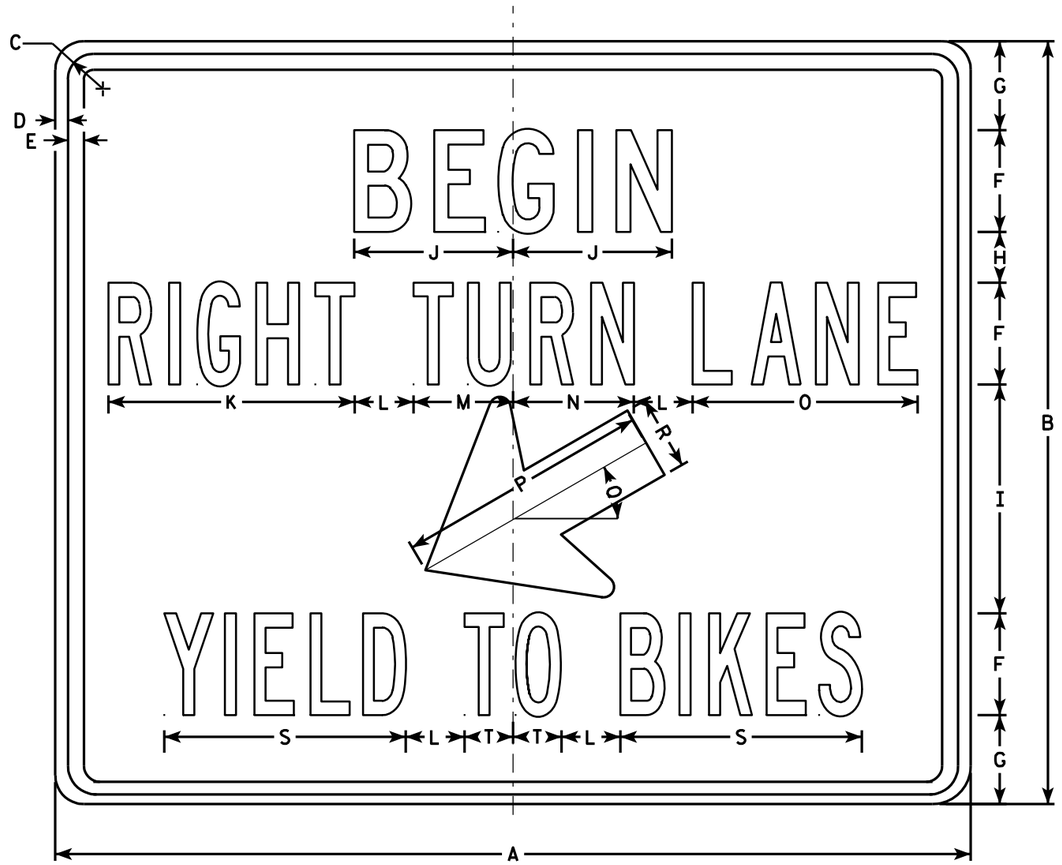
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/12/2011 PLATE NO. R3-17.2

PROJECT NO:

SHEET NO: 115 E



R4-4

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series C
   Lines 2 & 3 are Series B

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	30	1 3/8	1/2	5/8	4	3 1/2	2	9	6 1/4	9 3/4	2 3/8	3 7/8	4 3/4	8 7/8	10	30°	2 7/8	9 1/2	1 7/8							7.5
2M	36	30	1 3/8	1/2	5/8	4	3 1/2	2	9	6 1/4	9 3/4	2 3/8	3 7/8	4 3/4	8 7/8	10	30°	2 7/8	9 1/2	1 7/8							7.5
3																											
4																											
5																											

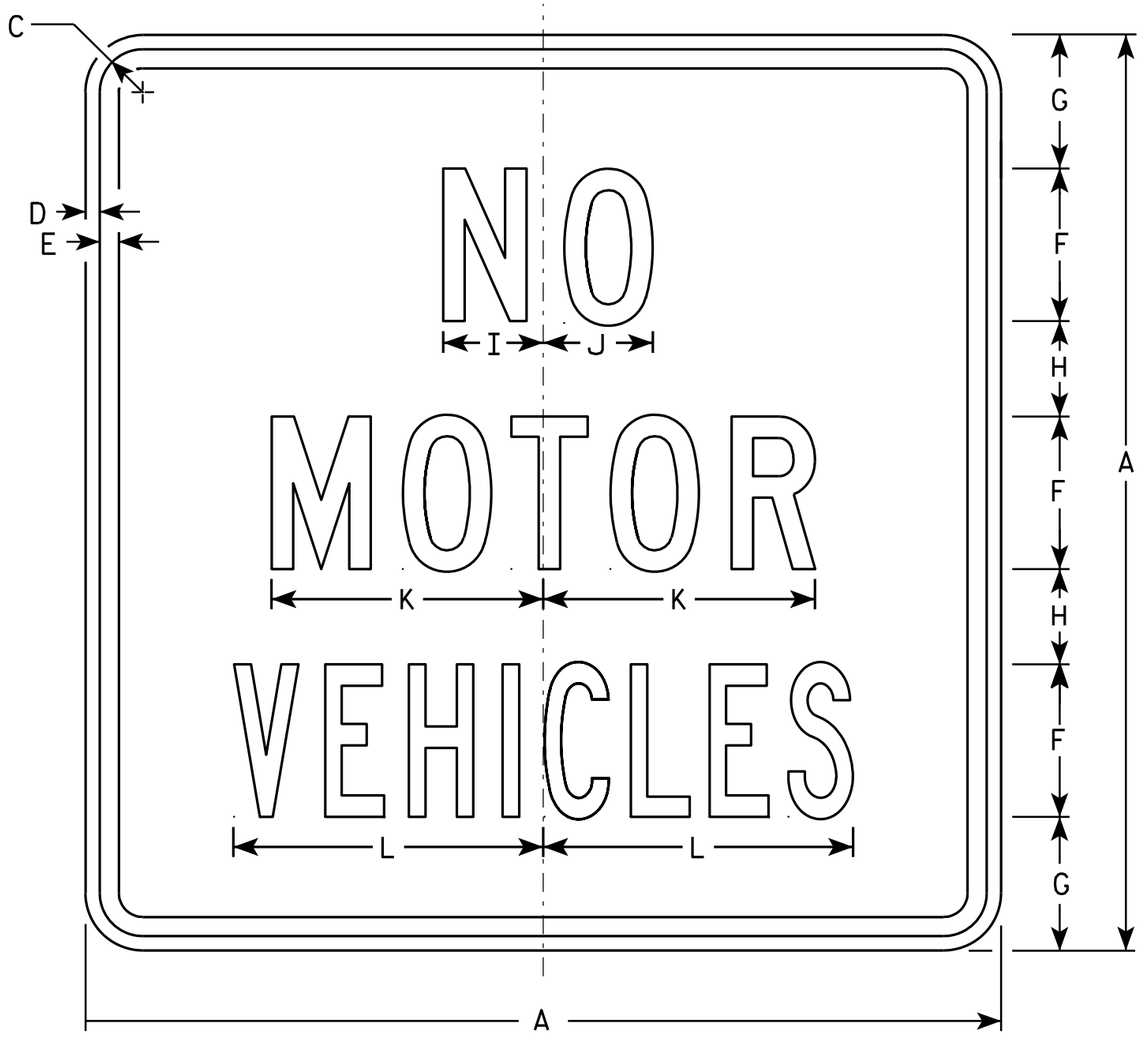
STANDARD SIGN
R4-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-4.2

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 116 | E



R5-3

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - See Note 5.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 & 2 are Series C.
   Line 3 is Series B.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24		1 1/8	3/8	1/2	4	3 1/2	2 1/2	2 5/8	2 7/8	7 1/8	8 1/8															4.0
2M	24		1 1/8	3/8	1/2	4	3 1/2	2 1/2	2 5/8	2 7/8	7 1/8	8 1/8															4.0
3																											
4																											
5																											

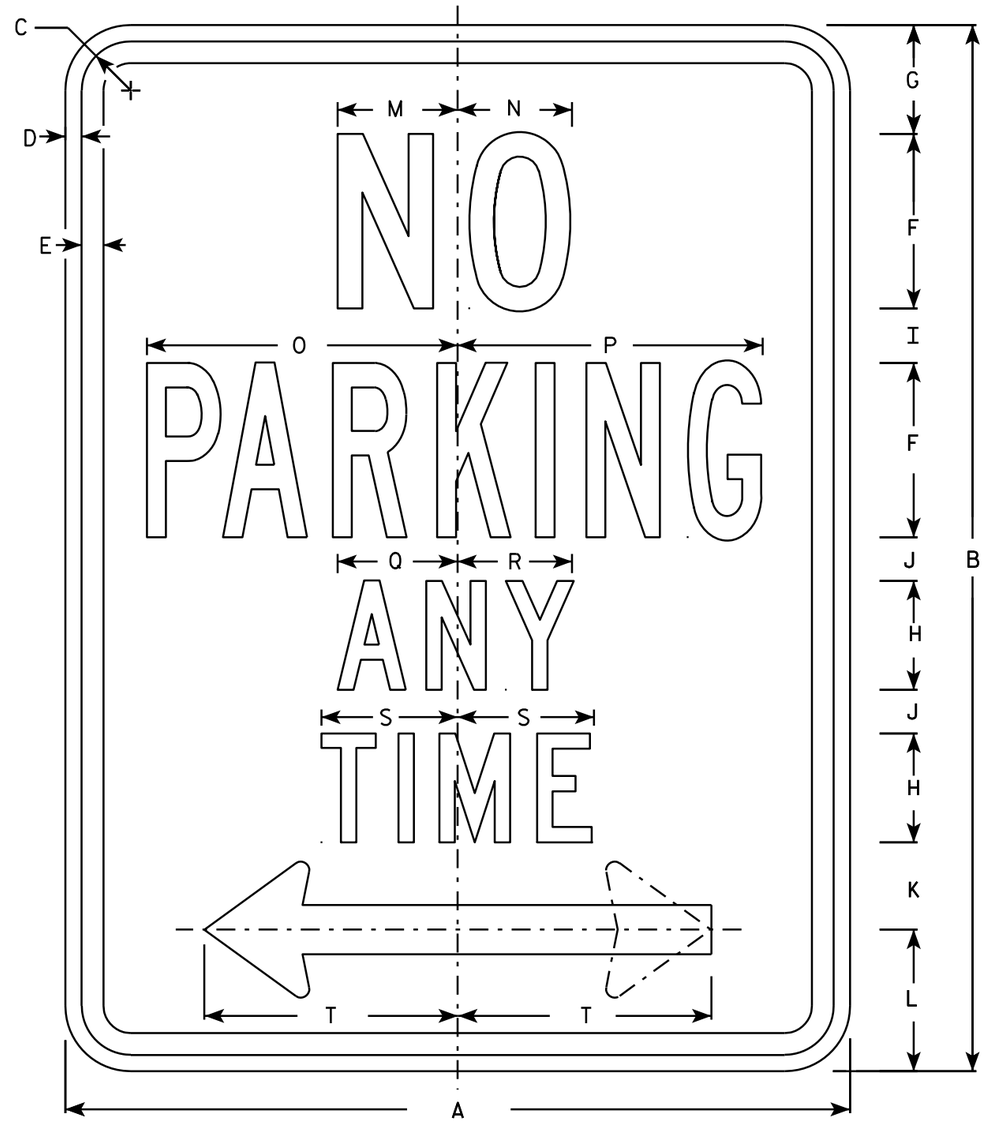
STANDARD SIGN
R5-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/29/2011 PLATE NO. R5-3.2

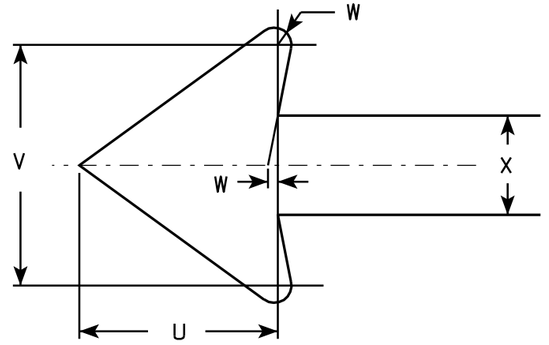
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 117 | E



R7-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)
   R7-1L (left arrow)
   R7-1R (right arrow)



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 118 | E

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R71.DGN PLOT DATE : 31-MAR-2011 09:20 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 3.476110:1.000000

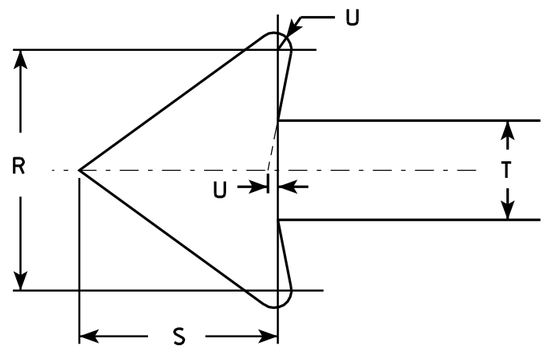
WISDOT/CADDS SHEET 42

R7-2

*- See Note 5

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 7
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals as required & adjust spacing to achieve proper balance.
6. R7-2D (double arrow)
   R7-2L (left arrow)
   R7-2R (right arrow)
7. Lines 1, 3 and 4 are series C, line 2 is series B.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	1 1/2	7/8	7/8	2	2 1/2	2	2	4 7/8	4 7/8	3 7/8	1 3/4	1 1/2	3/4	1/8						1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	5 7/8	2 5/8	2 1/4	1 1/8	1/4						3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	7 3/4	3 1/2	3	1 1/2	1/4						5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	7 3/4	3 1/2	3	1 1/2	1/4						5.0
4																											
5																											

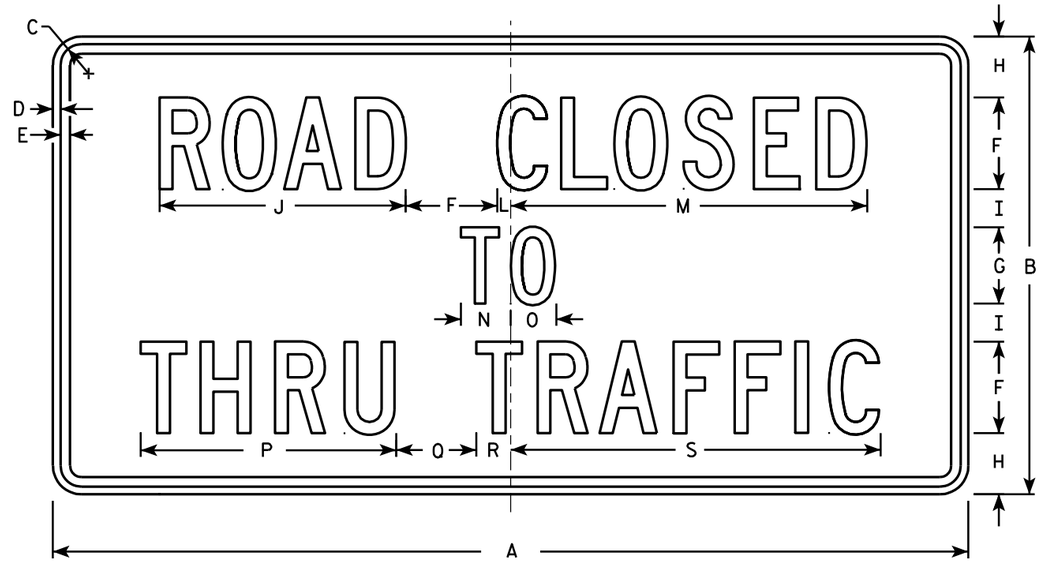
STANDARD SIGN
R7-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-2.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 119	E



R11-4

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

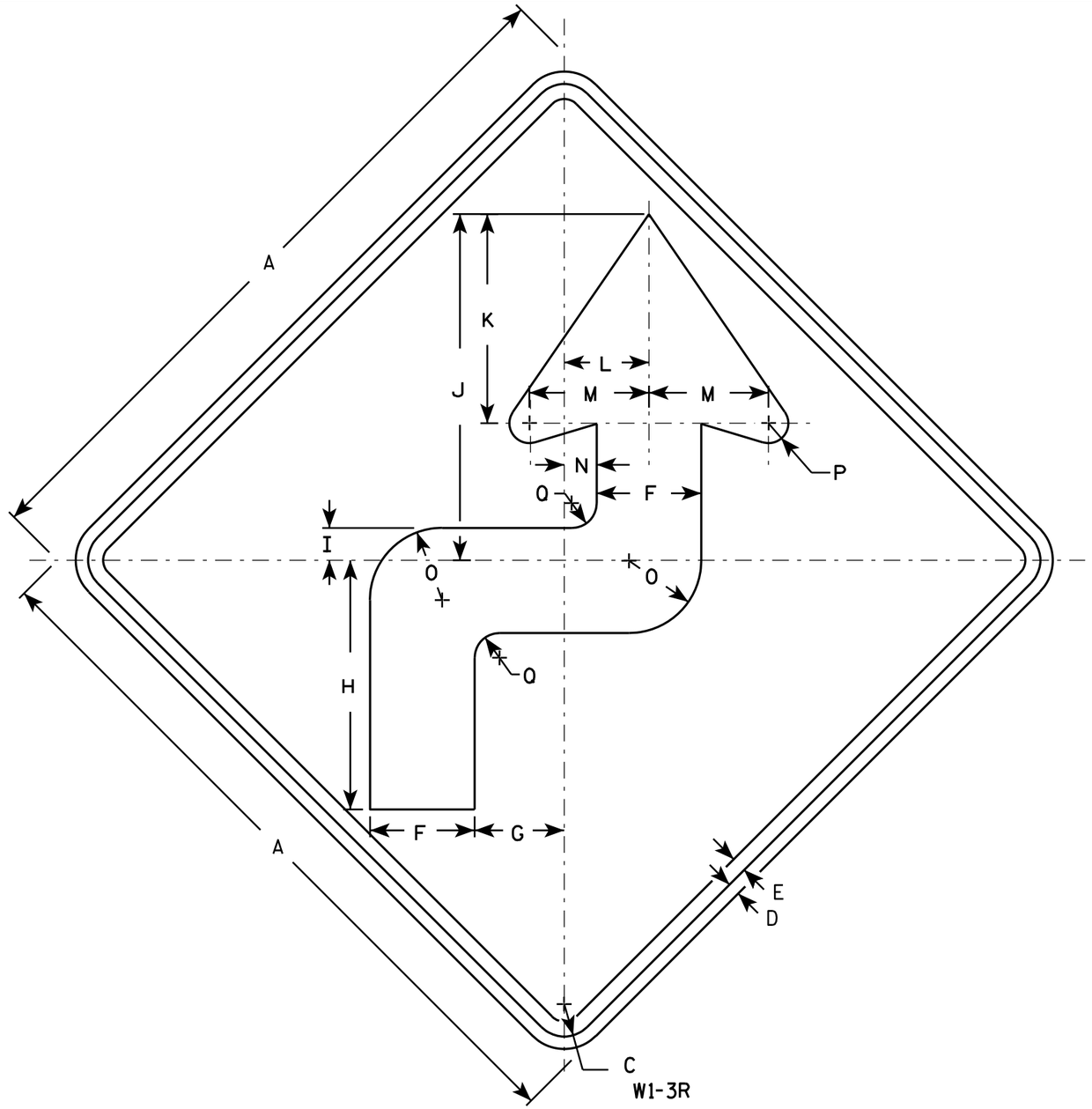
STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 120 | E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-3L is the same as W1-3R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	3	8 3/8	1 1/8	11 5/8	7	2 3/4	4	1 1/8	2 1/2	5/8	7/8										4.0
2S	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
2M	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
3	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
4	36		1 5/8	5/8	3/4	5 1/4	6	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
5	48		2 1/4	3/4	1	7	6	16 5/8	2 1/4	23 1/4	14	5 5/8	8	2 1/8	4 7/8	1 1/4	1 5/8										16.0

STANDARD SIGN
W1-3

WISCONSIN DEPT OF TRANSPORTATION

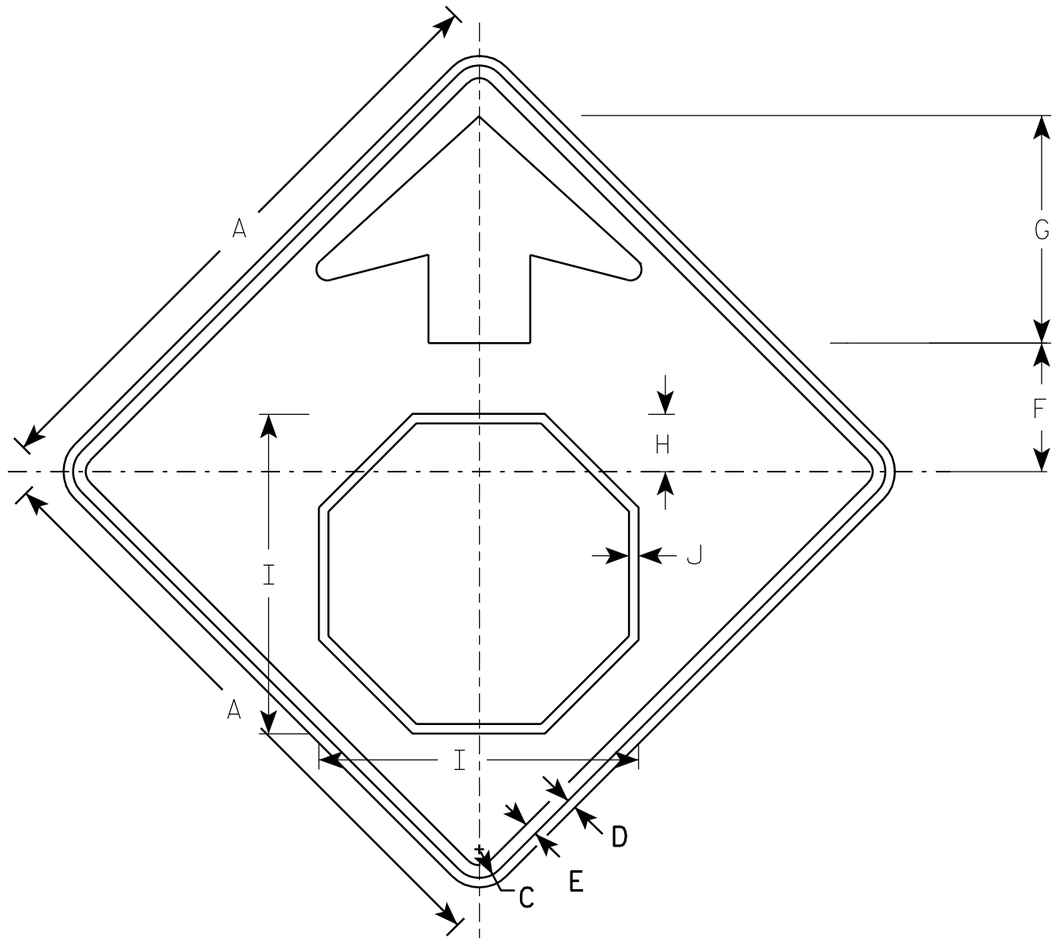
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-3.8

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 121 | E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W13.DGN PLOT DATE : 17-MAY-2012 13:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 6.698561:1.000000

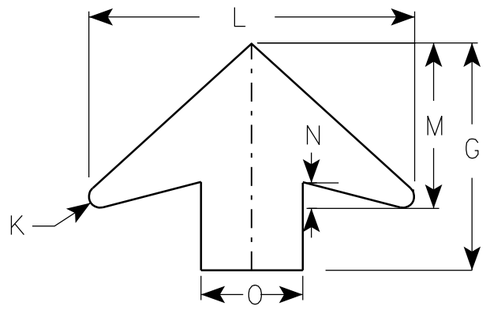
WISDOT/CADDS SHEET 42



W3-1

NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - YELLOW
   Arrow & Border - BLACK
   Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

STANDARD SIGN
W3-1

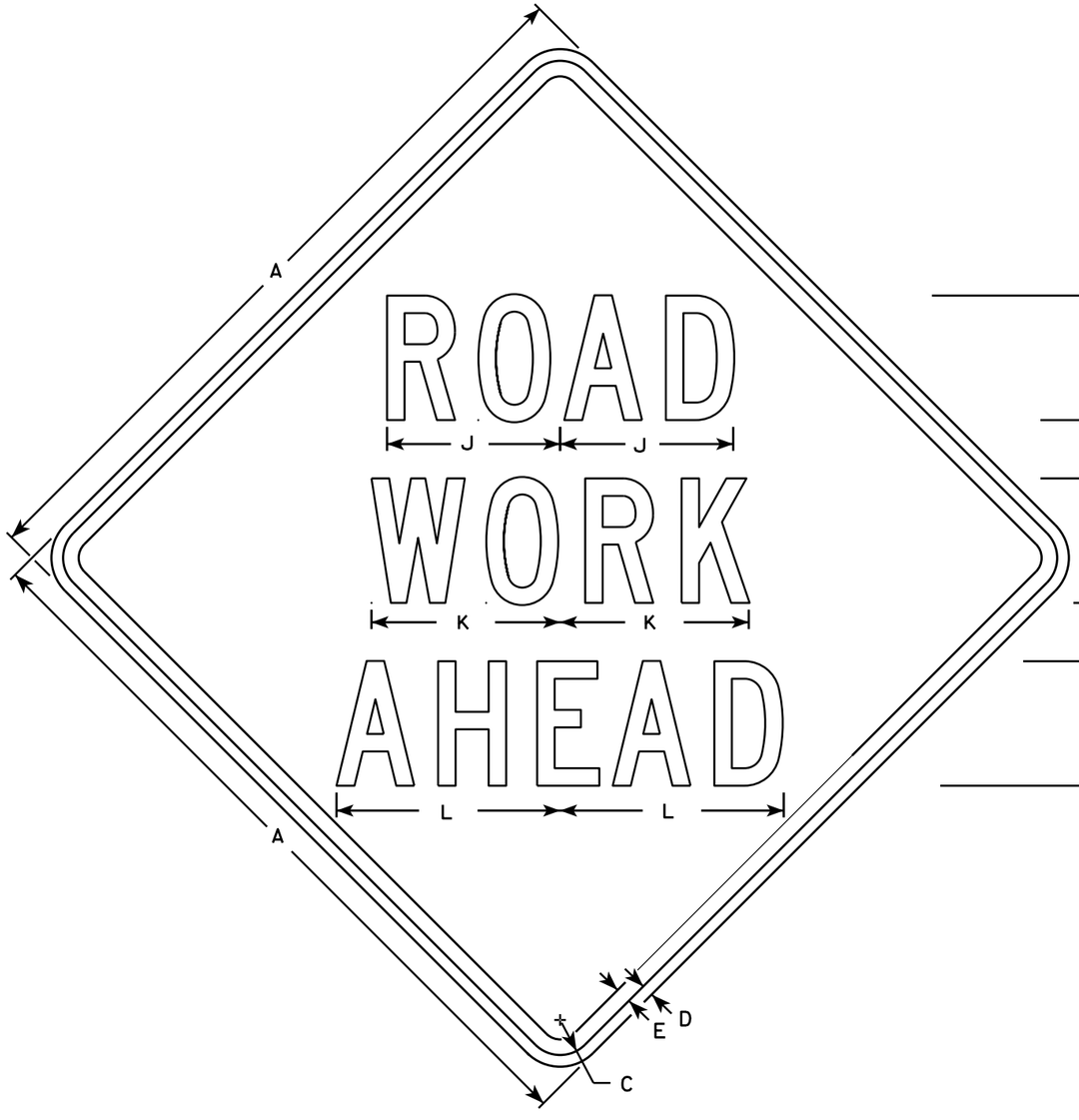
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

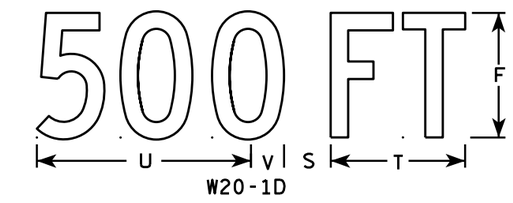
DATE 6/7/10 PLATE NO. W3-1.12

PROJECT NO:

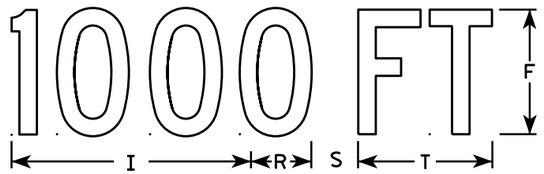
SHEET NO: 122 E



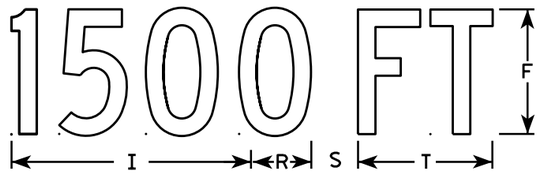
W20-1A



W20-1D



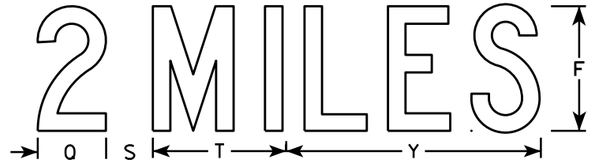
W20-1C



W20-1B

W20-1G

W20-1F



W20-1E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

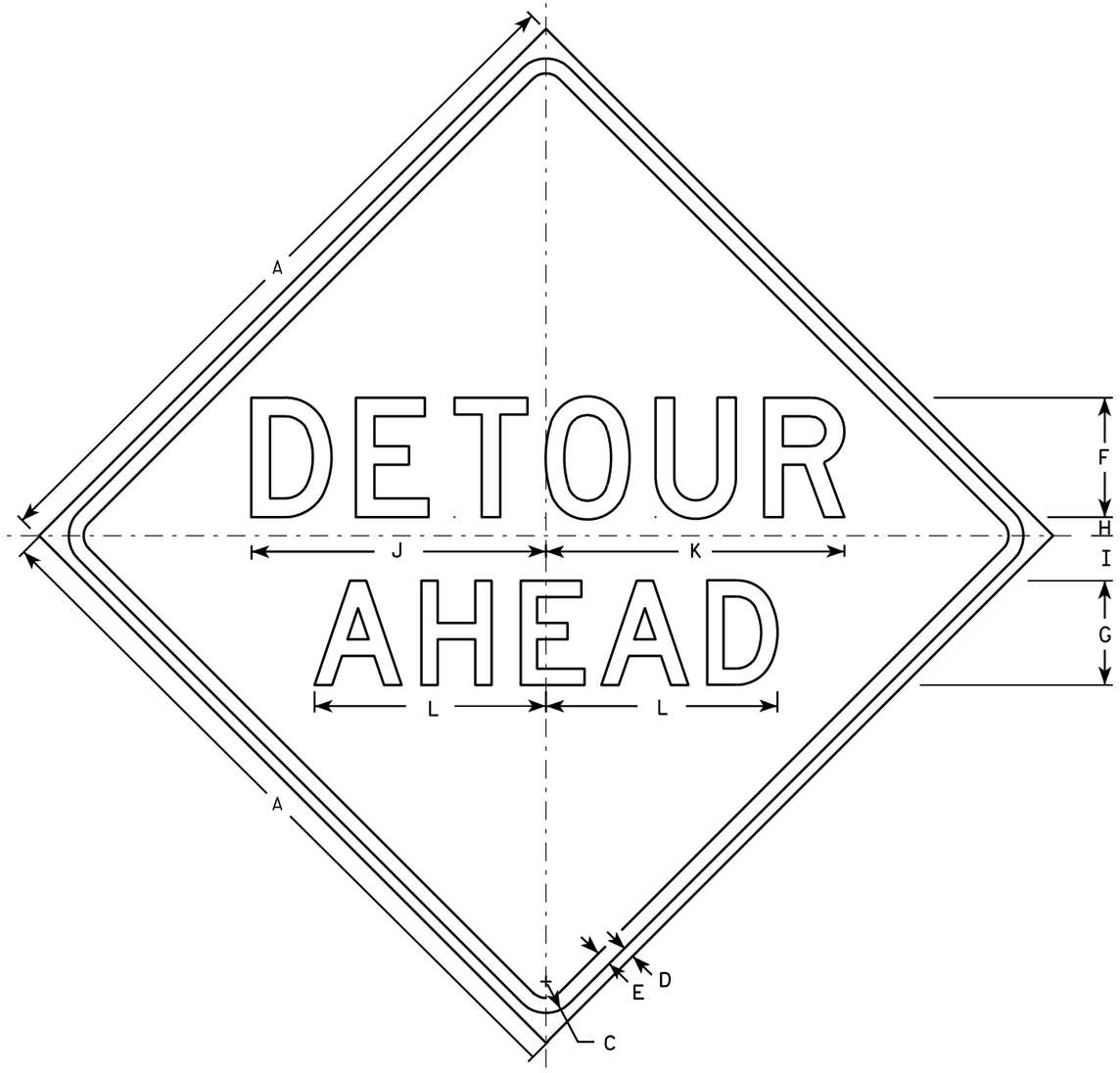
State Traffic Engineer

DATE 5/07/15 PLATE NO. W20-1.10

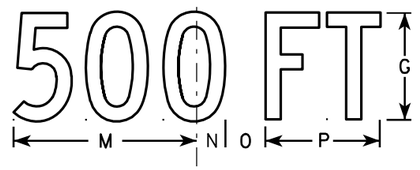
PROJECT NO: SHEET NO: E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W201.dgn PLOT DATE : 27-MAY-2015 15:58 PLOT BY : mscsja

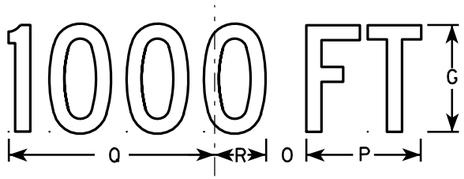
WISDOT/CADDS SHEET 42



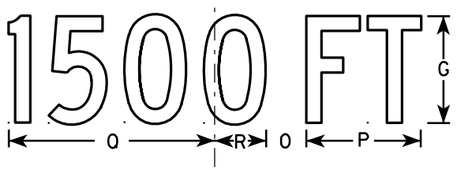
W20-2A



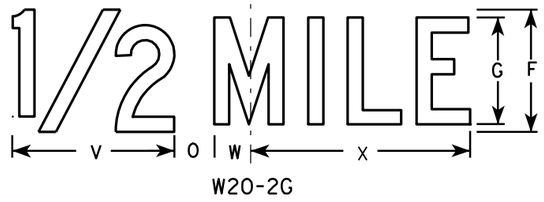
W20-2D



W20-2C



W20-2B



W20-2G

W20-2F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
   Line 2 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

STANDARD SIGN
W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

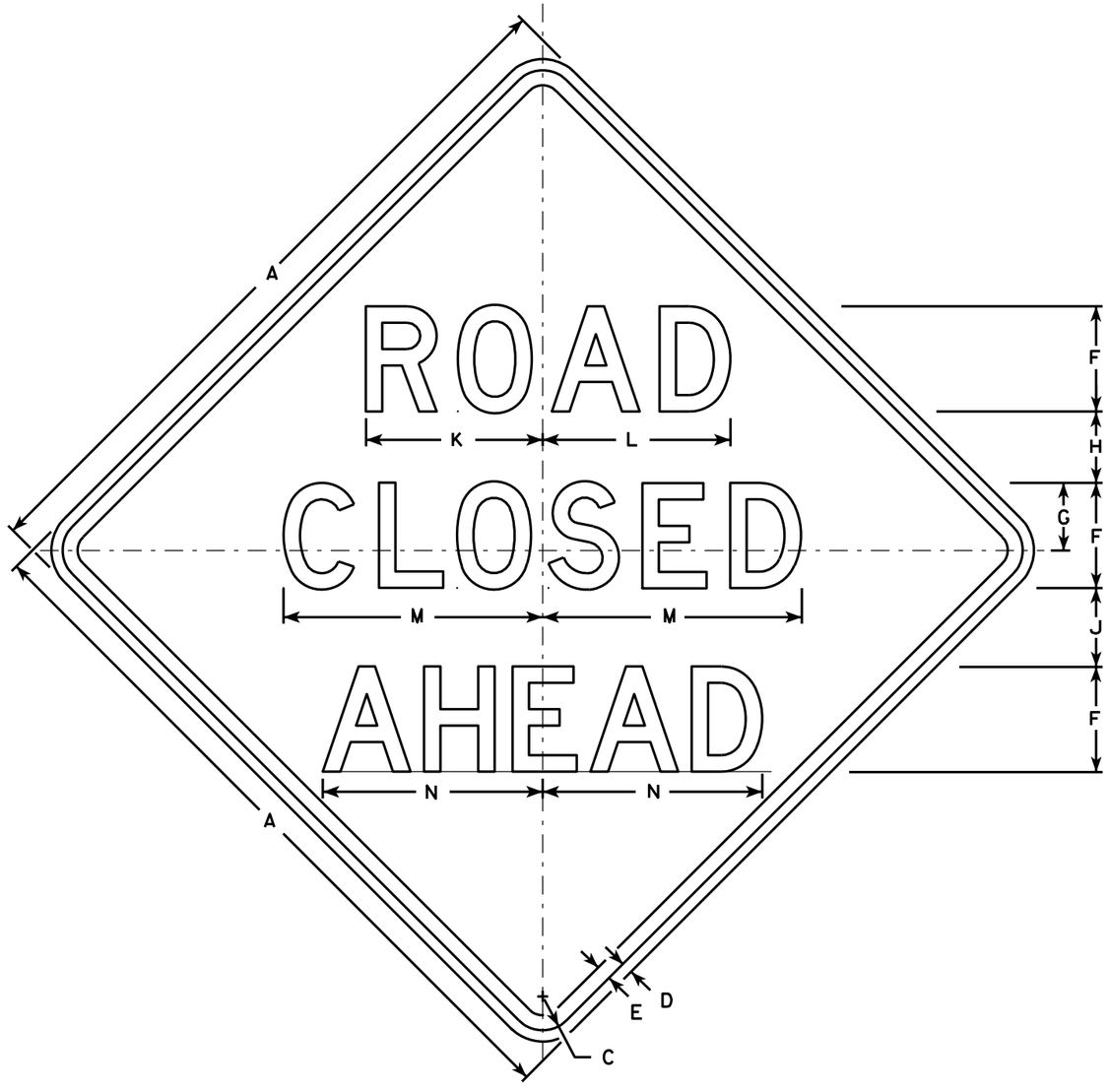
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

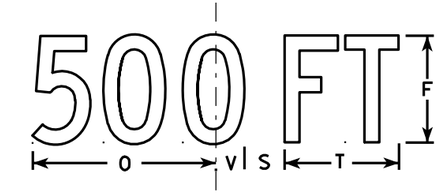
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 124 | E

7

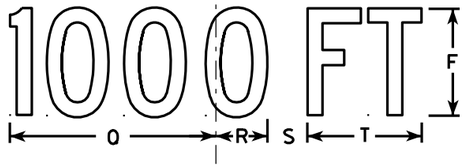
FILE NAME : C:\Users\PROJECTS\tr_stdplate\W202.DGN PLOT DATE : 18-MAR-2011 10:00 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42



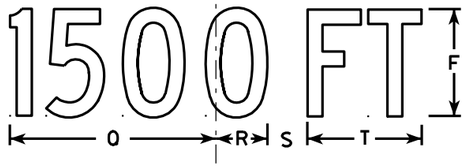
W20-3A



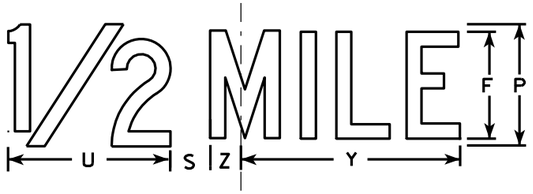
W20-3D



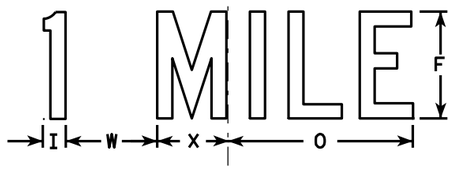
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
   Line 3 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 3/18/11 PLATE NO. W20-3.7

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 125 | E

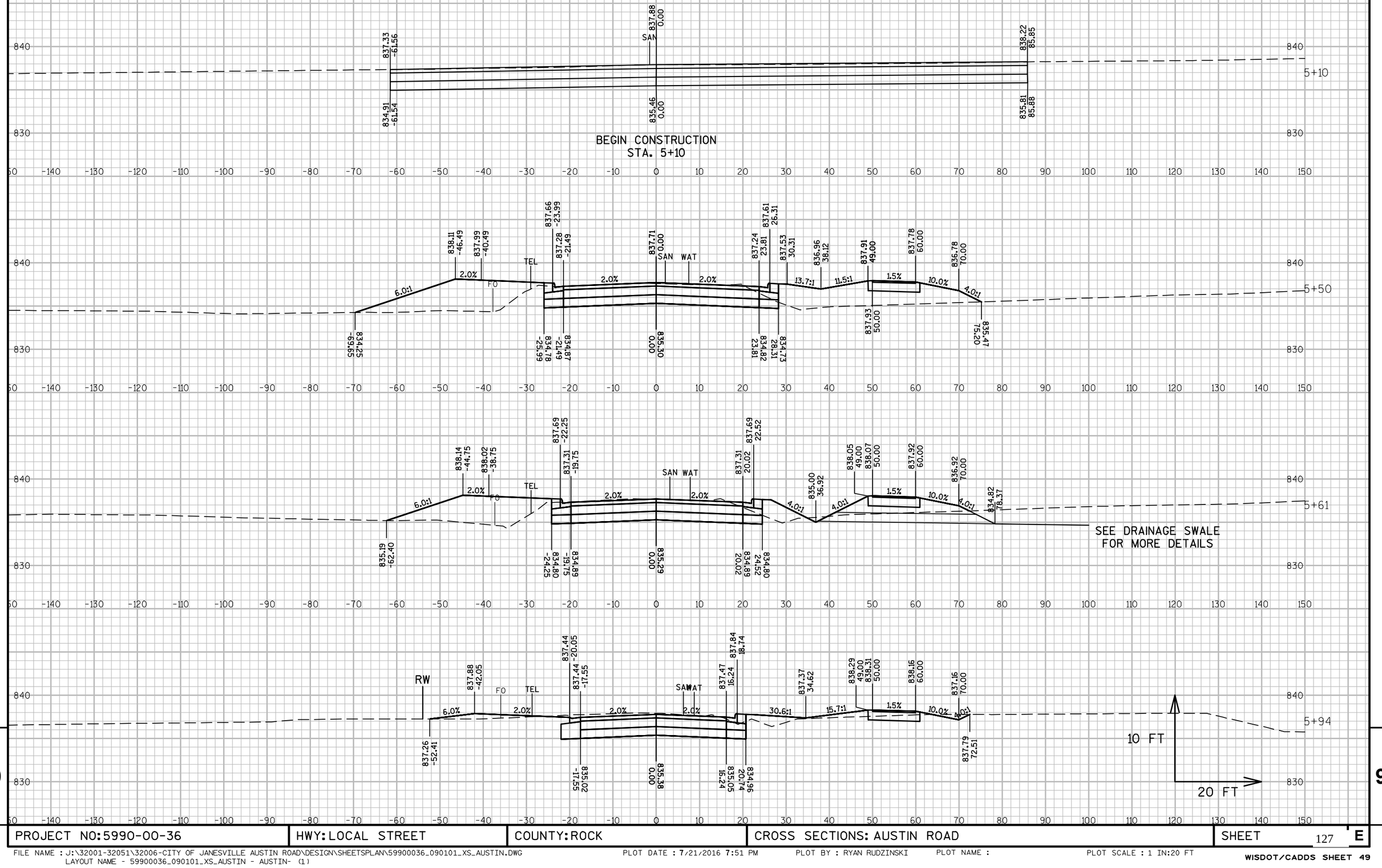
FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN PLOT DATE : 18-MAR-2011 12:08 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42

AUSTIN ROAD (5+10 - 31+50)

STATION	Real Station	Distance	AREA (SF) Cut	AREA (SF) Salvaged/Unusable Pavement Material	AREA (SF) Fill	Incremental Vol (CY) (Unadjusted) Cut Note 1	Incremental Vol (CY) (Unadjusted) Salvaged/Unusable Pavement Material Note 2	Incremental Vol (CY) (Unadjusted) Fill Note 3	Cumulative Vol (CY) Cut 1.00 Note 1	Cumulative Vol (CY) Expanded Fill 1.25	Mass Ordinate Note 8
05+10	510.00	0.00	709.00	6.25	1.72	0	0	0	0	0	0
05+50	550.00	40.00	305.00	6.25	194.01	751	9	145	751	181	561
05+61	561.00	11.00	105.02	6.25	133.83	84	3	67	835	265	558
05+94	594.00	33.00	116.89	6.25	27.53	136	8	99	970	388	563
06+00	600.00	6.00	94.75	6.25	42.34	24	1	8	994	398	575
06+50	650.00	50.00	94.44	6.25	39.29	175	12	76	1,169	492	644
07+00	700.00	50.00	103.15	6.25	59.22	183	12	91	1,352	606	702
07+50	750.00	50.00	105.13	6.25	90.23	193	12	138	1,545	779	710
08+00	800.00	50.00	90.14	6.25	132.72	181	12	206	1,726	1,037	621
08+50	850.00	50.00	86.84	6.25	134.49	164	12	247	1,889	1,346	464
09+00	900.00	50.00	94.51	6.25	133.74	168	12	248	2,057	1,657	310
09+50	950.00	50.00	77.68	6.25	138.32	159	12	252	2,217	1,972	143
10+00	1000.00	50.00	75.18	6.25	135.36	142	12	253	2,358	2,289	-44
10+50	1050.00	50.00	72.33	6.25	135.70	137	12	251	2,495	2,602	-232
11+00	1100.00	50.00	75.04	6.25	143.21	136	12	258	2,631	2,925	-430
11+50	1150.00	50.00	86.31	6.25	155.98	149	12	277	2,781	3,271	-639
12+00	1200.00	50.00	87.16	6.25	150.59	161	12	284	2,941	3,626	-845
12+20	1220.00	20.00	79.01	6.25	151.07	62	5	112	3,003	3,766	-927
12+50	1250.00	30.00	77.89	6.25	155.13	87	7	170	3,090	3,979	-1,060
13+00	1300.00	50.00	69.13	6.25	175.11	136	12	306	3,226	4,361	-1,317
13+40	1340.00	40.00	66.02	6.25	146.79	100	9	238	3,326	4,659	-1,525
13+50	1350.00	10.00	57.93	6.25	199.01	23	2	64	3,349	4,739	-1,584
14+00	1400.00	50.00	55.85	6.25	169.68	105	12	341	3,455	5,166	-1,917
14+50	1450.00	50.00	66.29	6.25	155.71	113	12	301	3,568	5,542	-2,192
15+00	1500.00	50.00	70.59	6.25	131.47	127	12	266	3,694	5,875	-2,409
15+50	1550.00	50.00	115.01	6.25	65.81	172	12	183	3,866	6,103	-2,477
16+00	1600.00	50.00	205.93	6.25	1.67	297	12	62	4,163	6,181	-2,270
16+50	1650.00	50.00	251.14	6.25	0.74	423	12	2	4,587	6,184	-1,861
17+00	1700.00	50.00	243.05	6.25	5.11	458	12	5	5,044	6,191	-1,422
17+50	1750.00	50.00	192.01	6.25	13.89	403	12	18	5,447	6,213	-1,053
18+00	1800.00	50.00	162.21	6.25	37.91	328	12	48	5,775	6,272	-796
18+50	1850.00	50.00	151.39	6.25	36.15	290	12	69	6,065	6,358	-603
18+87	1887.00	37.00	178.95	6.25	11.21	226	9	32	6,292	6,399	-426
19+00	1900.00	13.00	165.10	6.25	12.41	83	3	6	6,375	6,406	-353
19+50	1950.00	50.00	211.36	6.25	0.00	349	12	11	6,723	6,420	-30
20+00	2000.00	50.00	331.56	6.25	0.00	503	12	0	7,226	6,420	461
20+50	2050.00	50.00	425.10	6.25	0.00	701	12	0	7,926	6,420	1,150
20+65	2065.00	15.00	448.05	6.25	0.00	243	3	0	8,169	6,420	1,389
21+00	2100.00	35.00	411.57	6.25	0.03	557	8	0	8,726	6,420	1,938
21+50	2150.00	50.00	338.55	6.25	43.36	695	12	40	9,421	6,470	2,571
22+00	2200.00	50.00	232.98	6.25	76.21	529	12	111	9,950	6,609	2,950
22+50	2250.00	50.00	162.89	6.25	39.30	367	12	107	10,316	6,743	3,171
23+00	2300.00	50.00	114.67	6.25	78.60	257	12	109	10,573	6,879	3,280
23+17.54	2317.54	17.54	119.83	6.25	69.19	76	4	48	10,650	6,939	3,292
23+50	2350.00	32.46	91.87	6.25	61.91	127	8	79	10,777	7,038	3,313
24+00	2400.00	50.00	92.18	6.25	44.89	170	12	99	10,947	7,161	3,349
24+50	2450.00	50.00	126.44	6.25	15.71	202	12	56	11,150	7,231	3,469
24+75	2475.00	25.00	126.16	6.25	12.02	117	6	13	11,267	7,247	3,564
25+00	2500.00	25.00	139.19	6.25	12.65	123	6	11	11,390	7,262	3,667
25+50	2550.00	50.00	218.88	6.25	1.03	332	12	13	11,721	7,277	3,971
25+75	2575.00	25.00	215.87	6.25	1.87	201	6	1	11,922	7,279	4,165
26+00	2600.00	25.00	182.14	6.25	10.58	184	6	6	12,107	7,286	4,336
26+50	2650.00	50.00	219.17	6.25	0.45	372	12	10	12,478	7,299	4,684
26+75	2675.00	25.00	222.17	6.25	0.02	204	6	0	12,683	7,299	4,882
27+00	2700.00	25.00	227.24	6.25	1.67	208	6	1	12,891	7,300	5,083
27+38	2738.00	38.00	312.65	6.25	0.00	380	9	1	13,270	7,302	5,453
27+50	2750.00	12.00	305.65	6.25	0.00	137	3	0	13,408	7,302	5,588
28+00	2800.00	50.00	270.53	6.25	6.36	533	12	6	13,941	7,309	6,102
28+50	2850.00	50.00	185.32	6.25	13.83	422	12	19	14,363	7,333	6,489
28+70.01	2870.01	20.01	143.98	6.25	18.77	122	5	12	14,485	7,348	6,592
29+00	2900.00	29.99	130.11	6.25	36.15	152	7	31	14,638	7,386	6,699
29+35	2935.00	35.00	161.26	6.25	13.32	189	8	32	14,827	7,426	6,839
29+60	2960.00	25.00	196.49	6.25	8.05	166	6	10	14,992	7,438	6,987
30+00	3000.00	40.00	229.42	6.25	25.21	315	9	25	15,308	7,469	7,262
30+32	3032.00	32.00	236.24	6.25	2.95	276	7	17	15,584	7,490	7,510
30+50	3050.00	18.00	231.20	6.25	4.01	156	4	2	15,739	7,493	7,659
31+00	3100.00	50.00	200.74	6.25	5.19	400	12	9	16,139	7,503	8,036
31+50	3150.00	50.00	252.65	6.25	23.46	420	12	27	16,559	7,537	8,411
						16,559	611	6,029			

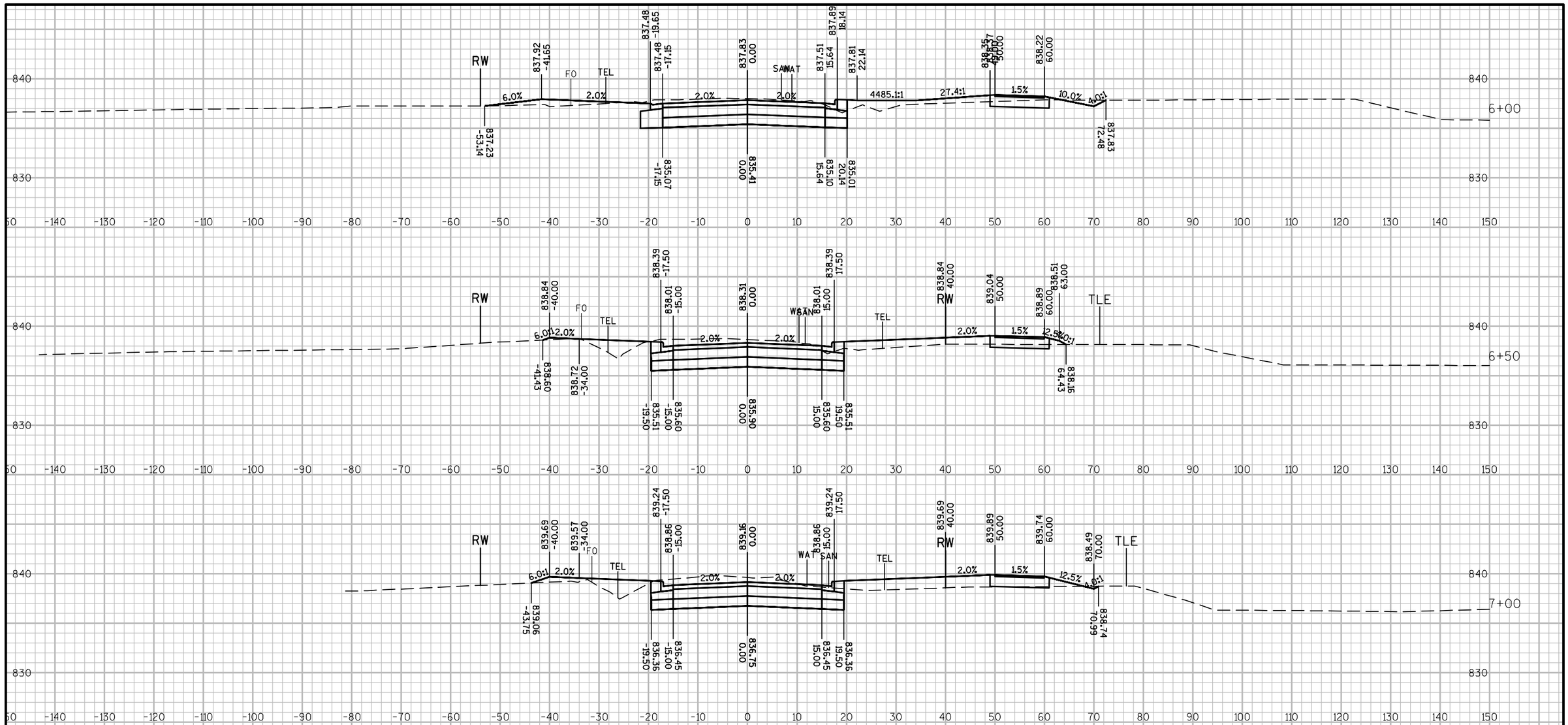
KNOLLVIEW (100+50 - 101+50)

STATION	Real Station	Distance	AREA (SF) Cut	AREA (SF) Salvaged/Unusable Pavement Material	AREA (SF) Fill	Incremental Vol (CY) (Unadjusted) Cut Note 1	Incremental Vol (CY) (Unadjusted) Salvaged/Unusable Pavement Material Note 2	Incremental Vol (CY) (Unadjusted) Fill Note 3	Cumulative Vol (CY) Cut 1.00 Note 1	Cumulative Vol (CY) Expanded Fill 1.25	Mass Ordinate Note 8
100+50	10050.00	0.00	21.75	7.33	77.66	0	0	0	0	0	0
101+00	10100.00	50.00	37.30	7.33	116.95	55	14	180	55	225	-184
101+30	10130.00	30.00	77.27	7.33	85.90	64	8	113	118	366	-270
101+50	10150.00	20.00	51.09	7.33	71.00	48	5	58	166	439	-300
						166	27	351			



PROJECT NO:5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 127 E

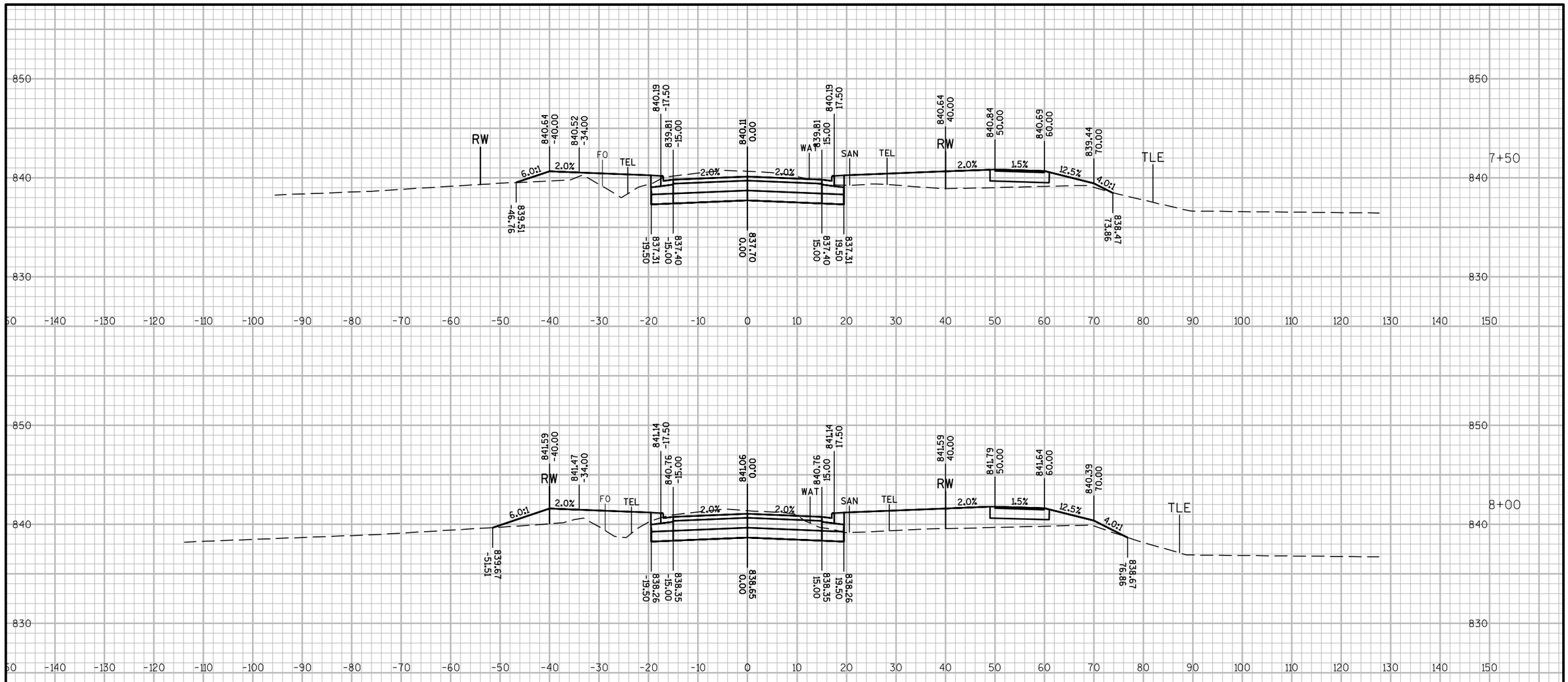
FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (1)
PLOT DATE : 7/21/2016 7:51 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 128 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (2)

PLOT DATE : 7/21/2016 7:52 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT



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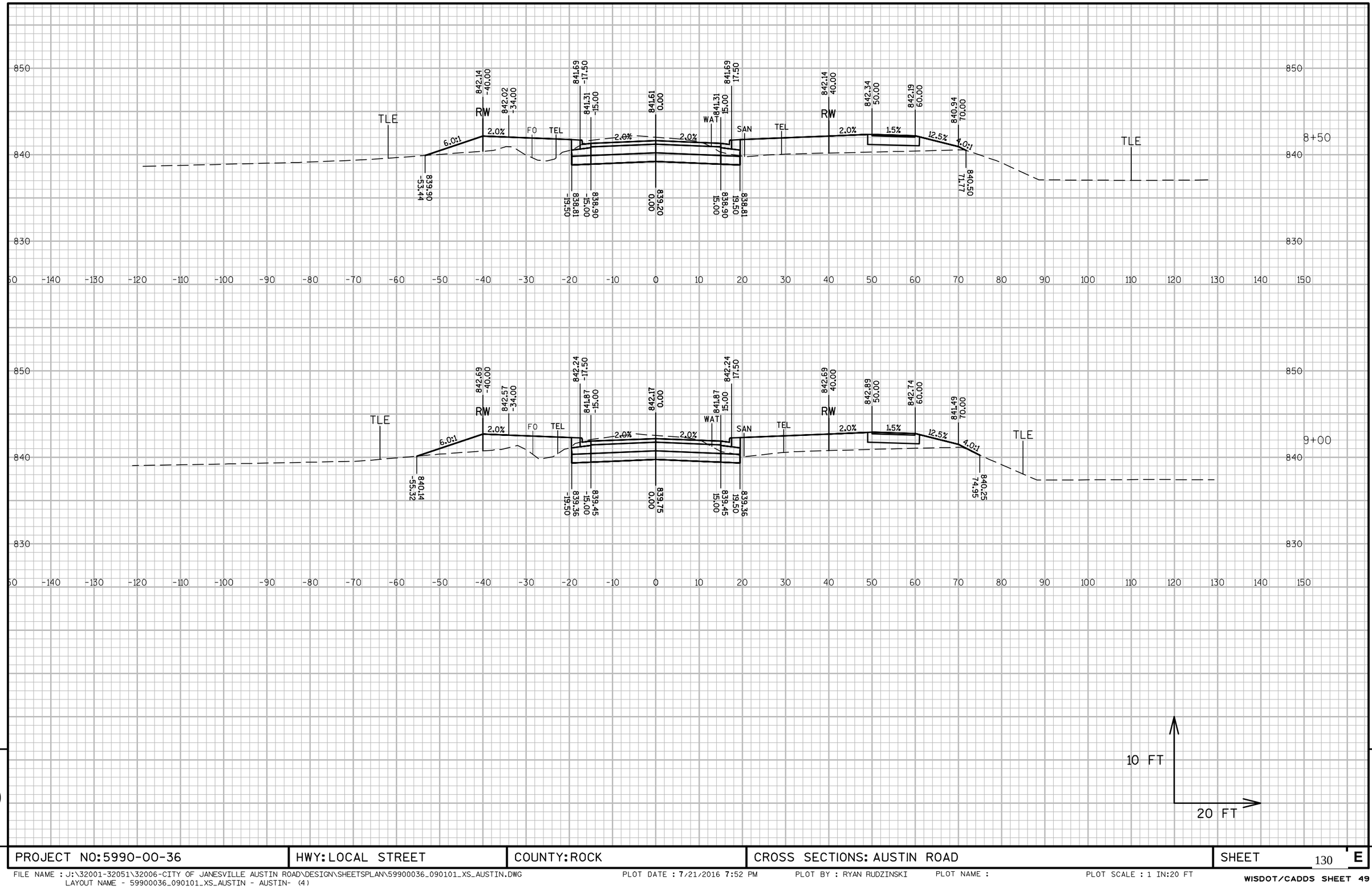
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 129 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (3)
PLOT DATE : 7/21/2016 7:52 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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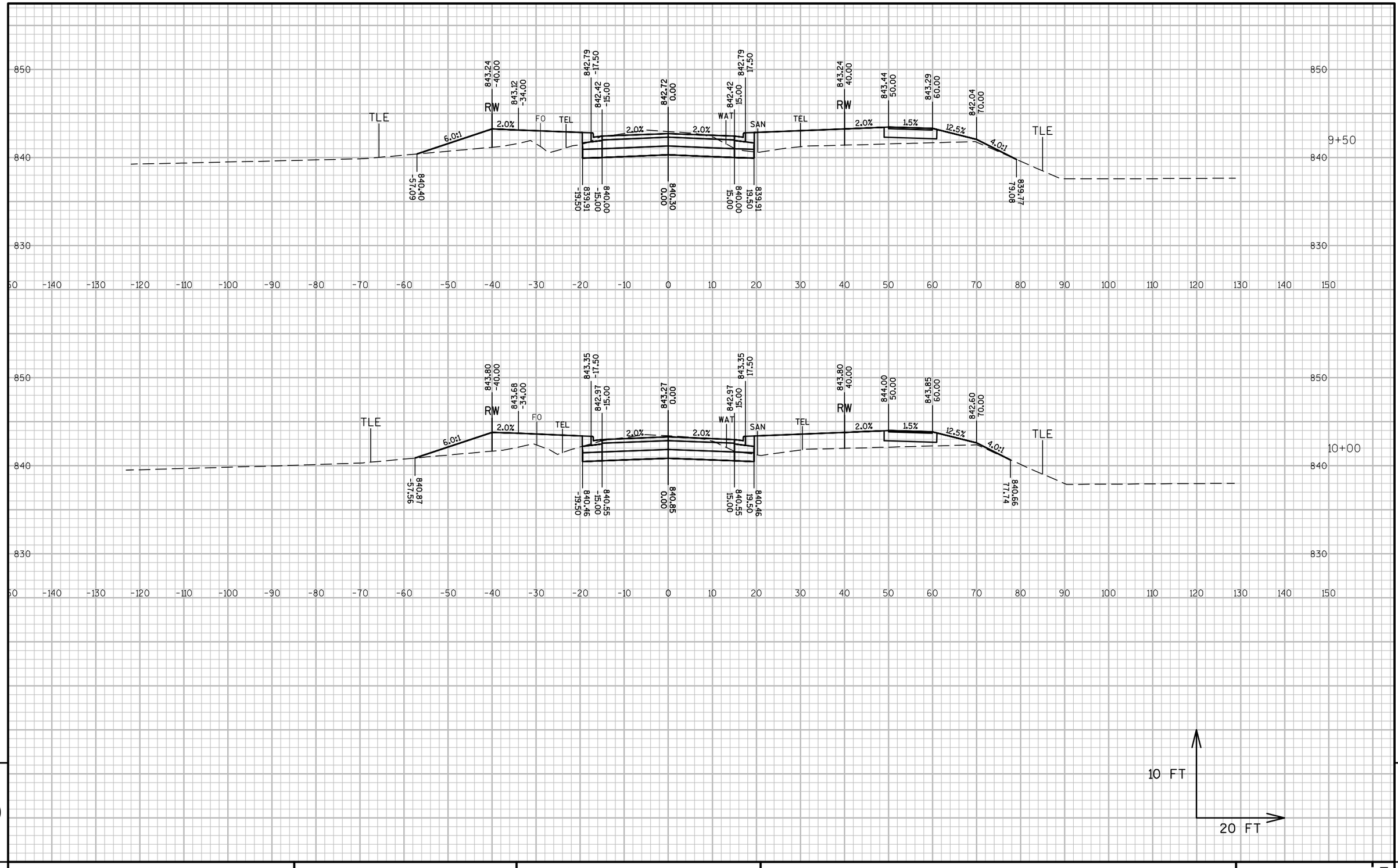
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 130 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (4)
PLOT DATE : 7/21/2016 7:52 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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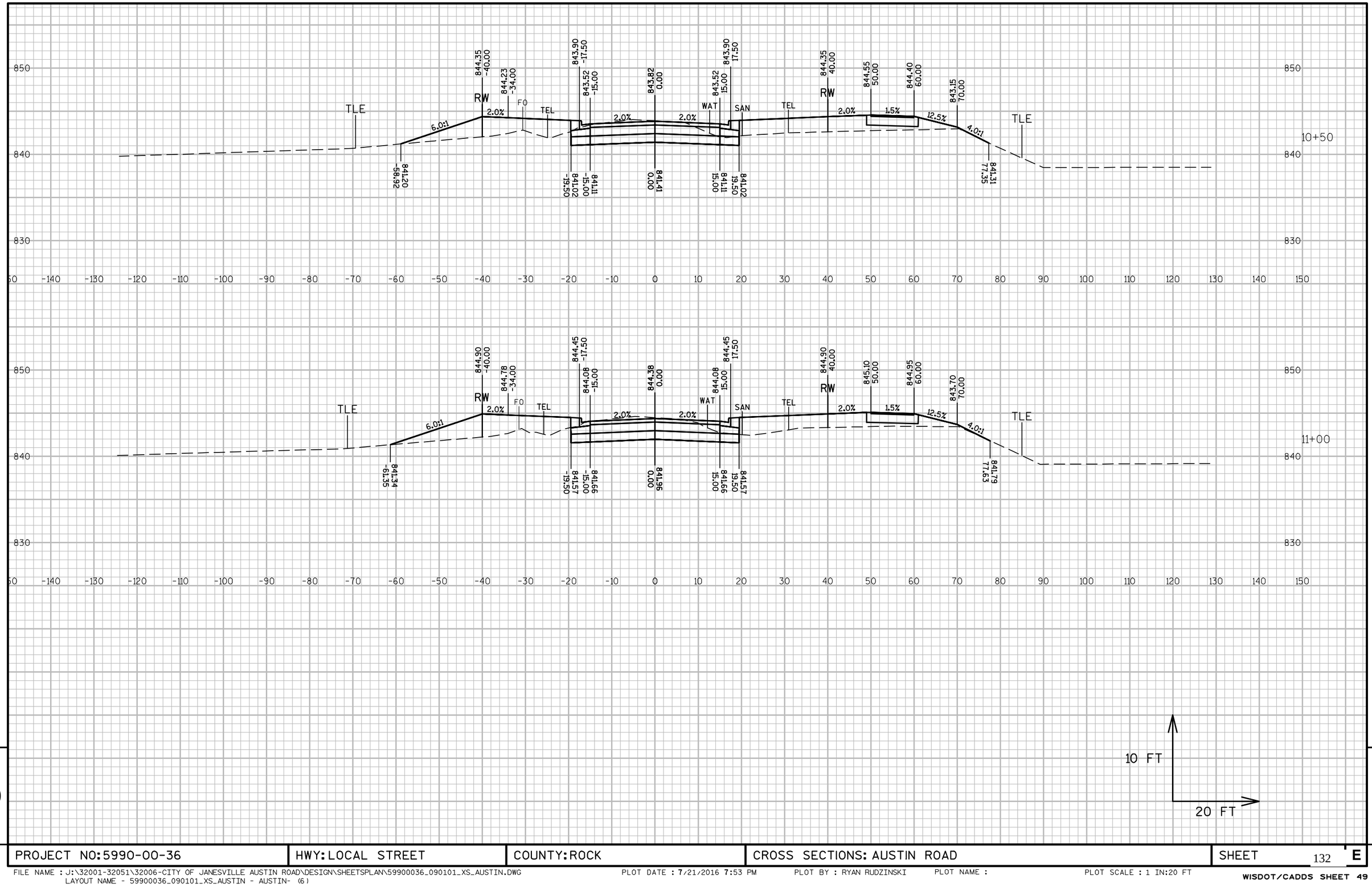
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 131 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (5)
PLOT DATE : 7/21/2016 7:52 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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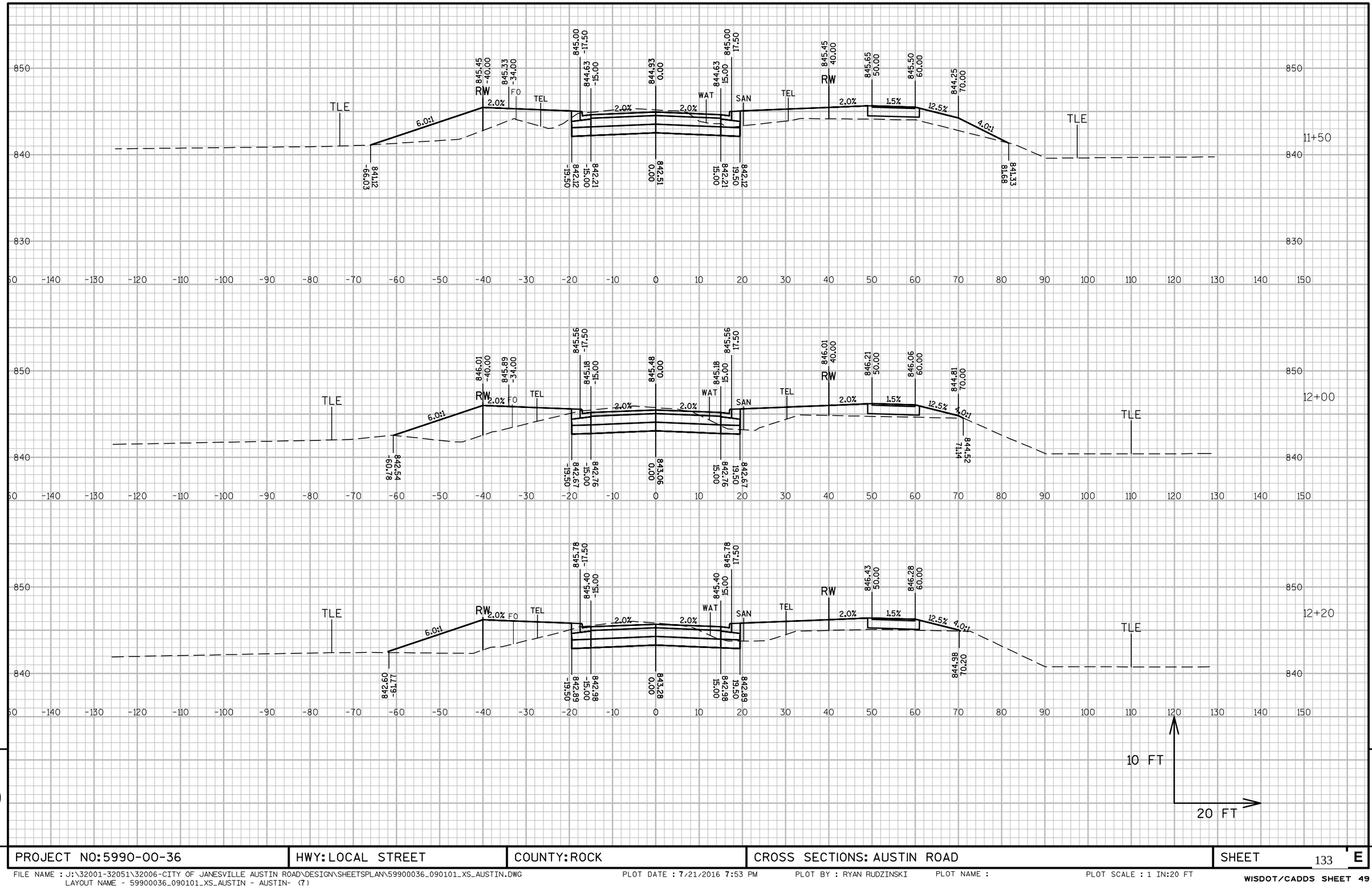
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 132 E

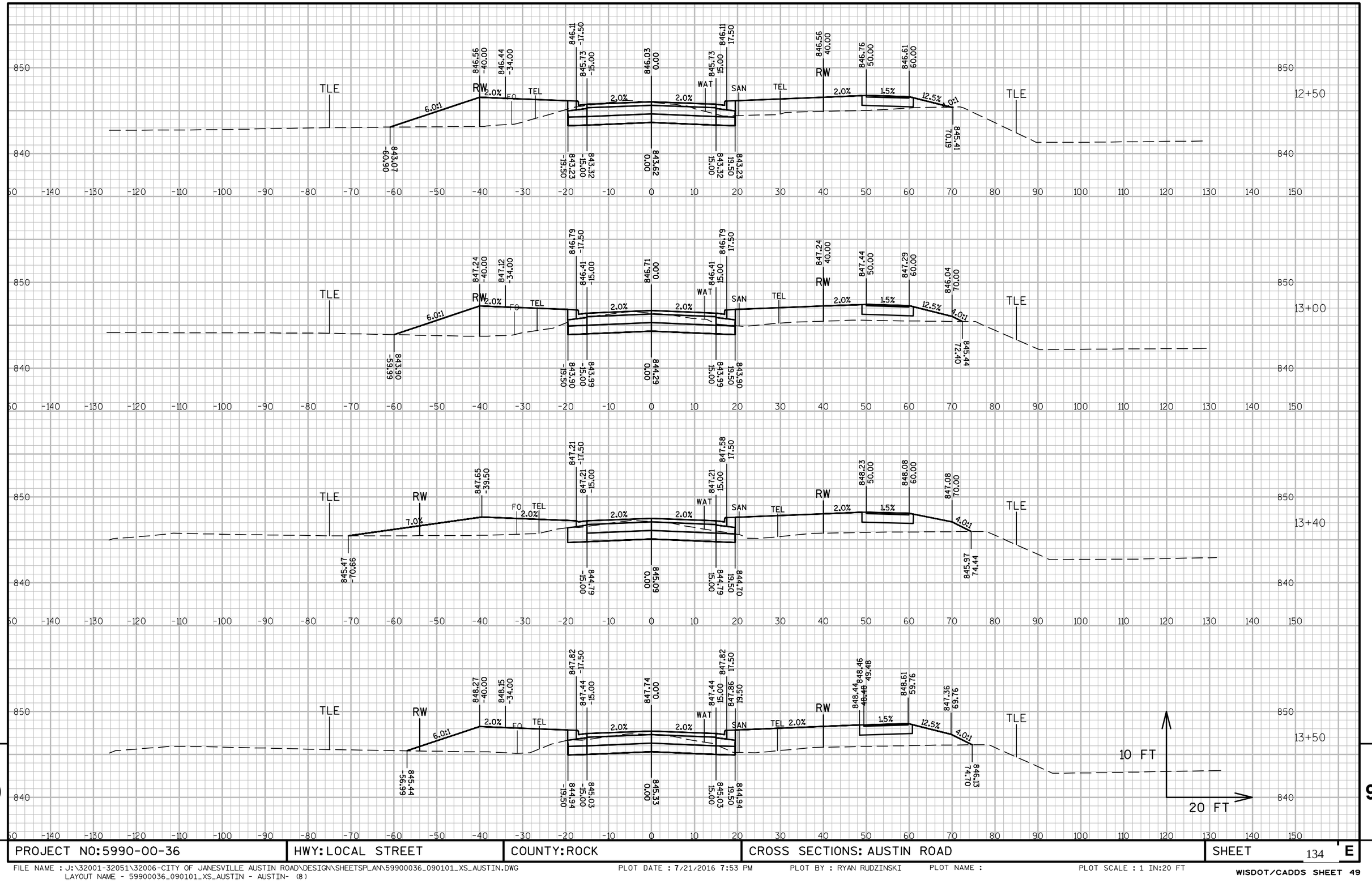


PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 133 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (7)

PLOT DATE : 7/21/2016 7:53 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 49



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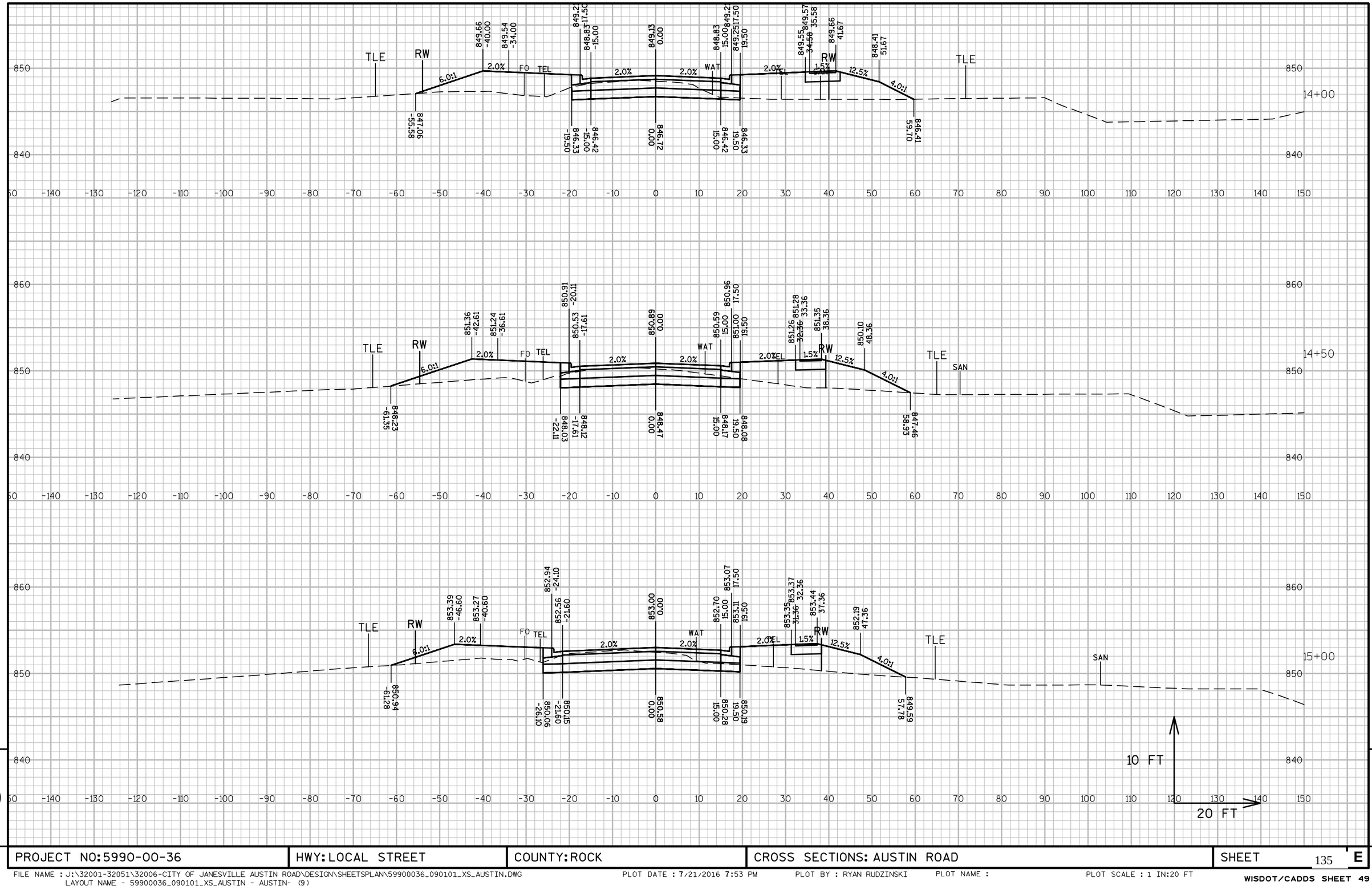
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 134 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (8)
PLOT DATE : 7/21/2016 7:53 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49

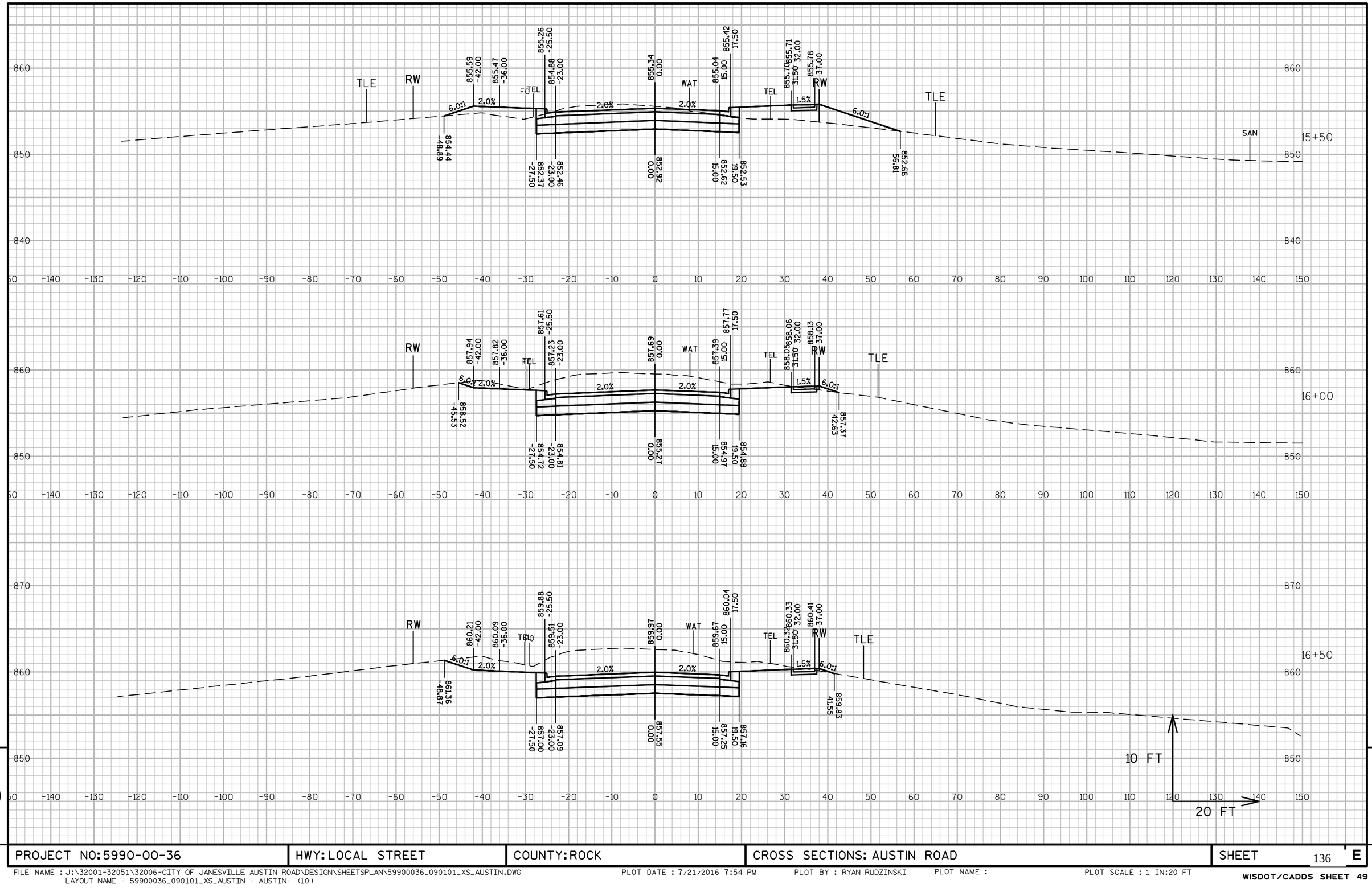


PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 135 E

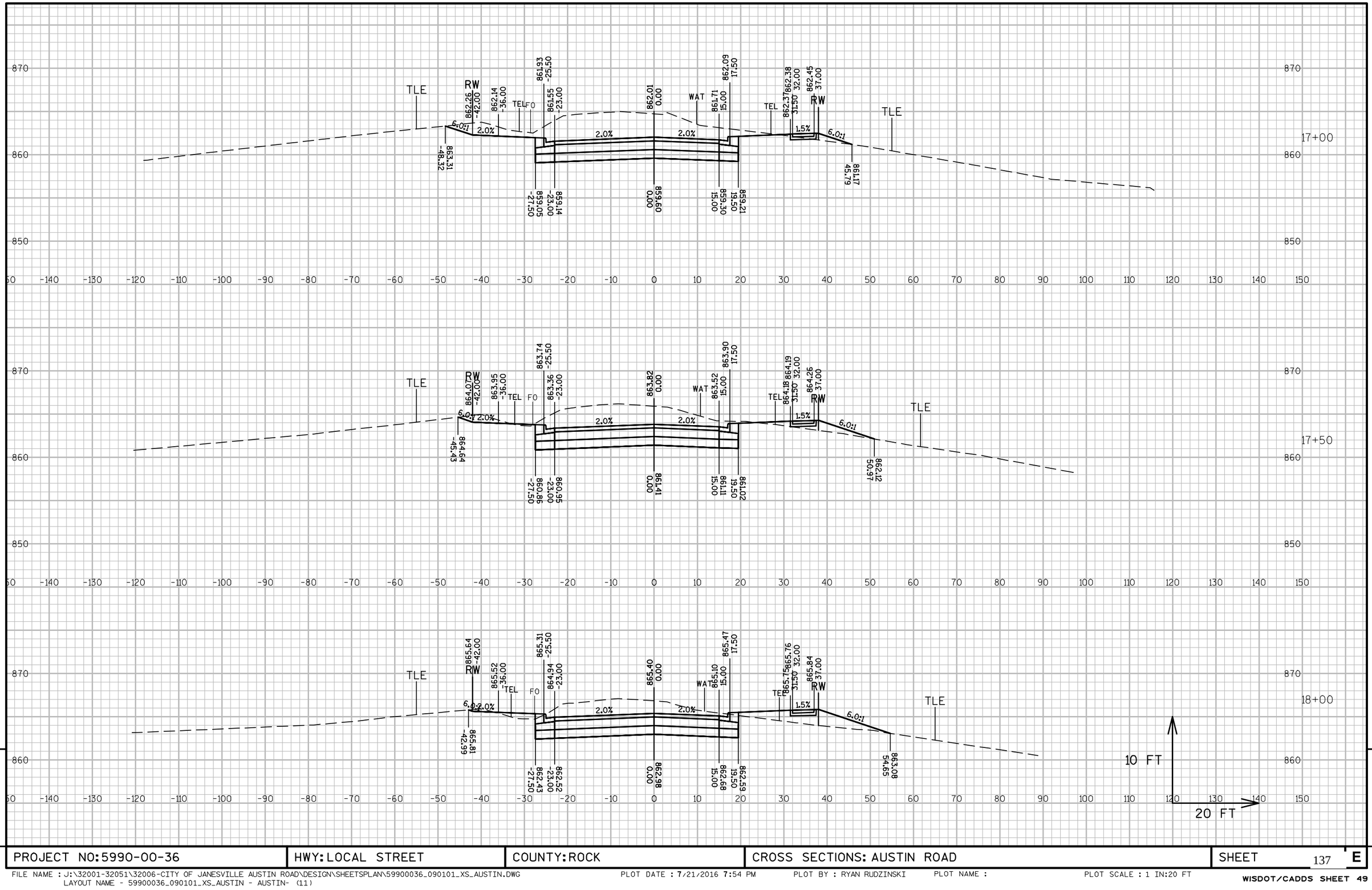
FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (9)

PLOT DATE : 7/21/2016 7:53 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 49



PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 136 E



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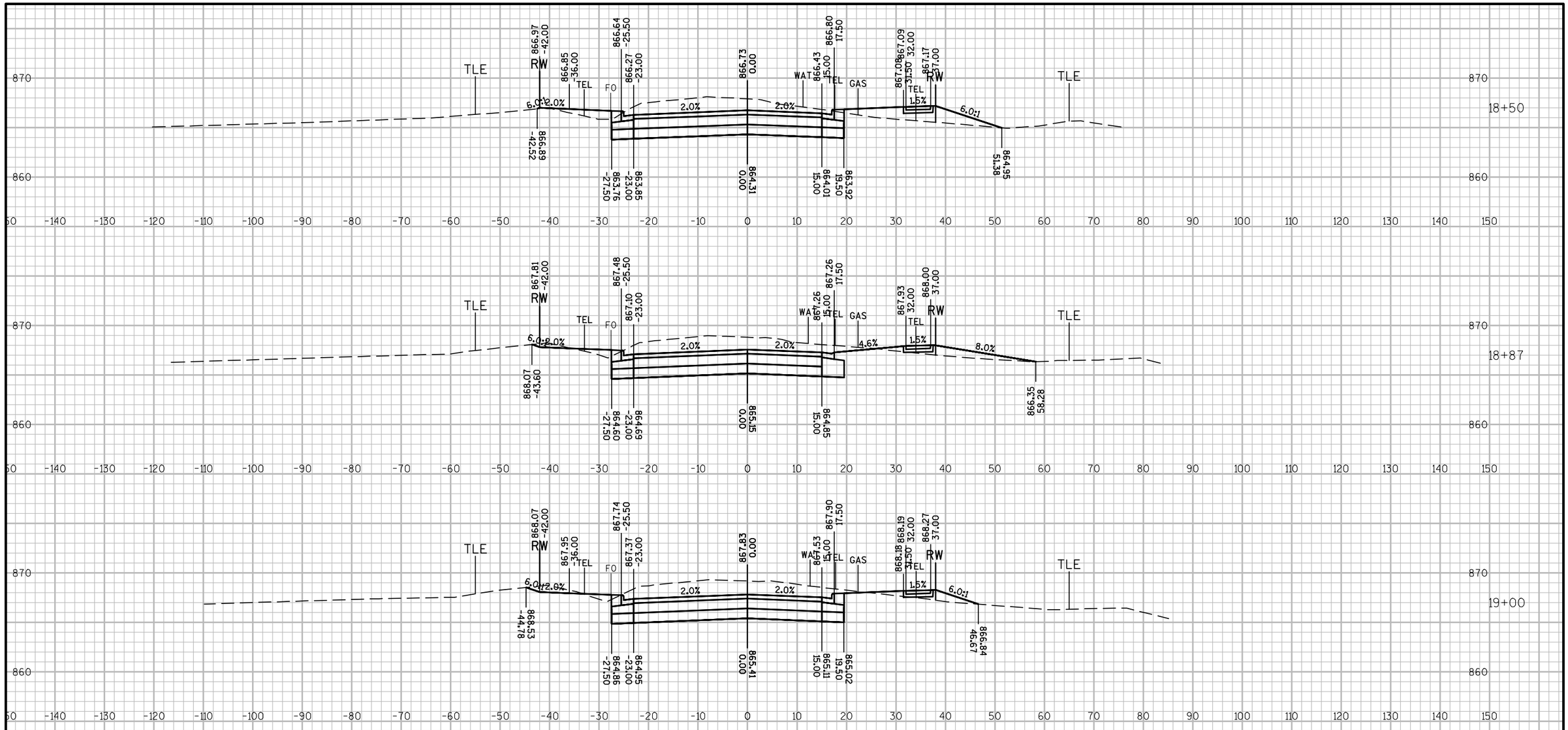
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 137 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (11)
PLOT DATE : 7/21/2016 7:54 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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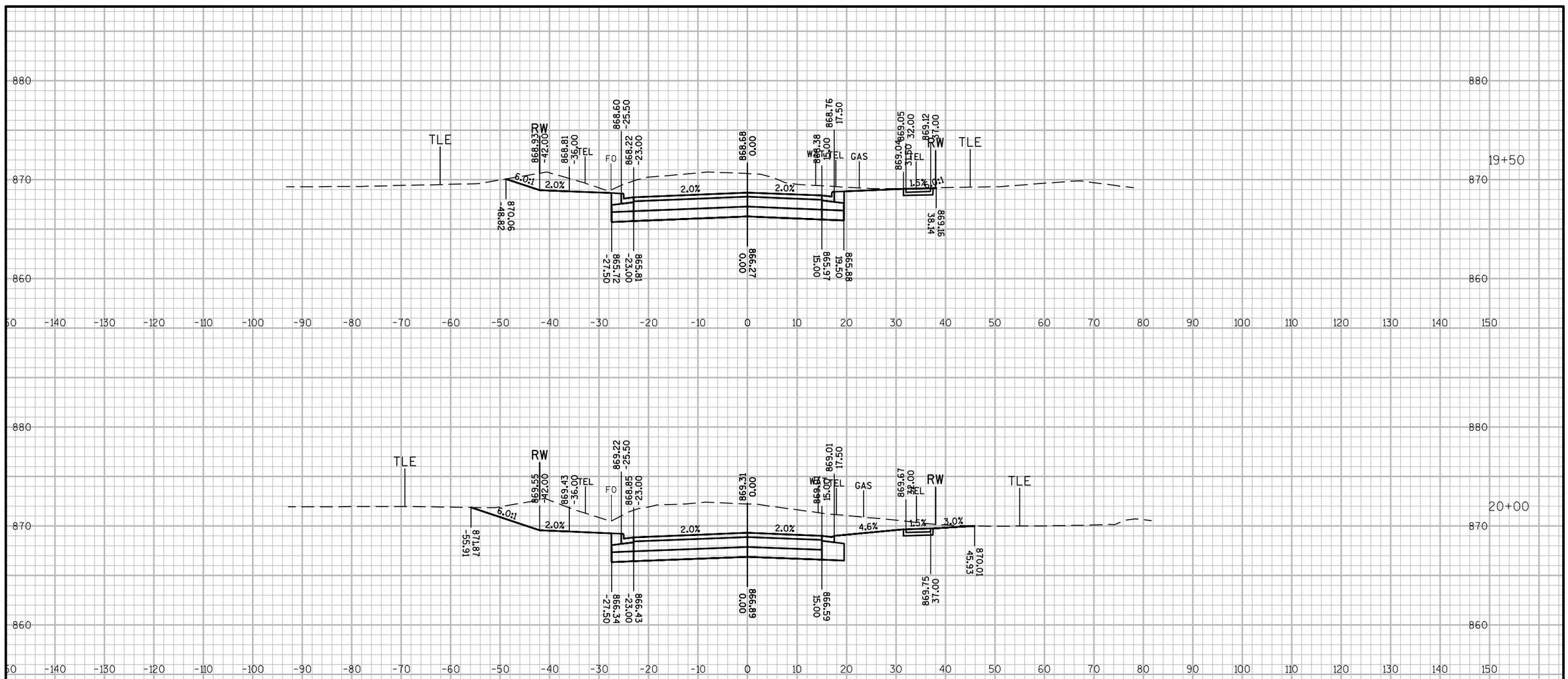
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 138 E

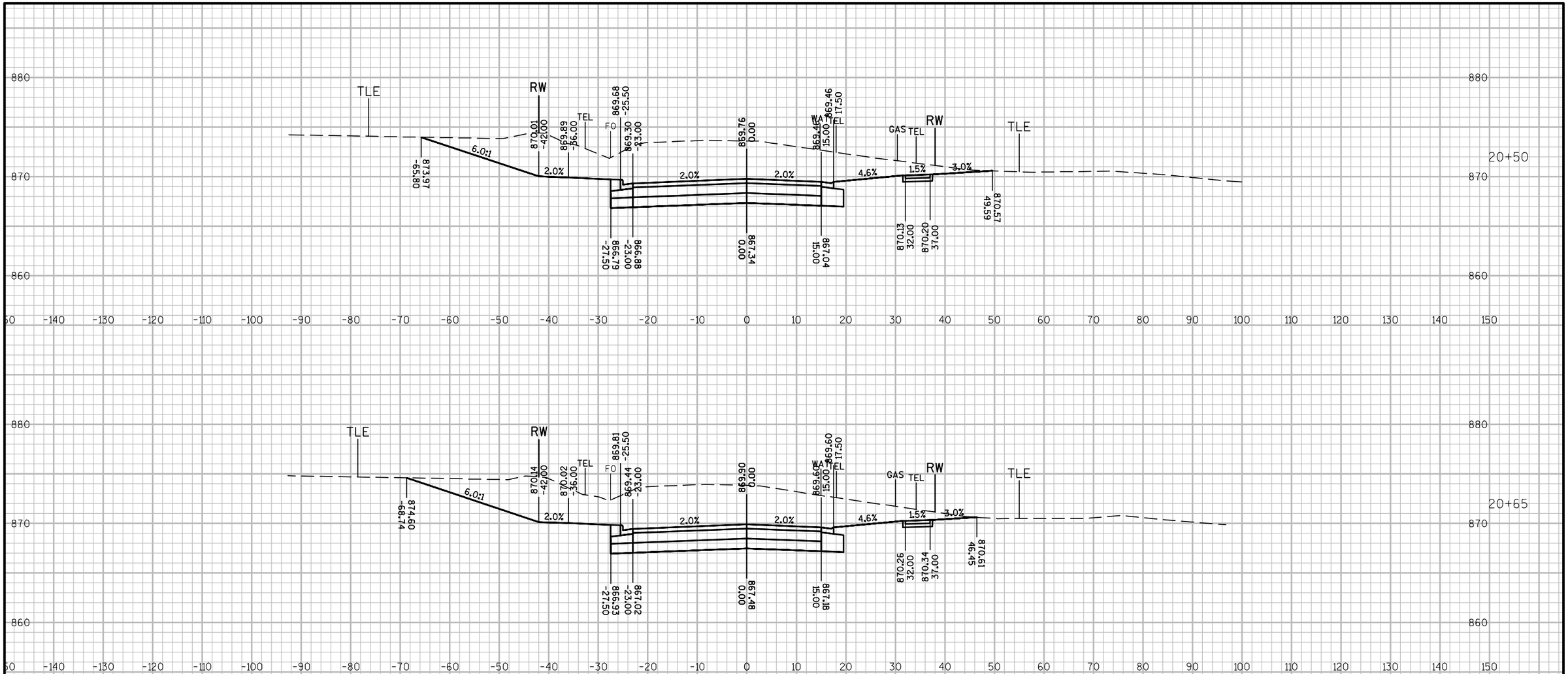
FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (12)
PLOT DATE : 7/21/2016 7:54 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 139 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (13)

PLOT DATE : 7/21/2016 7:54 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT



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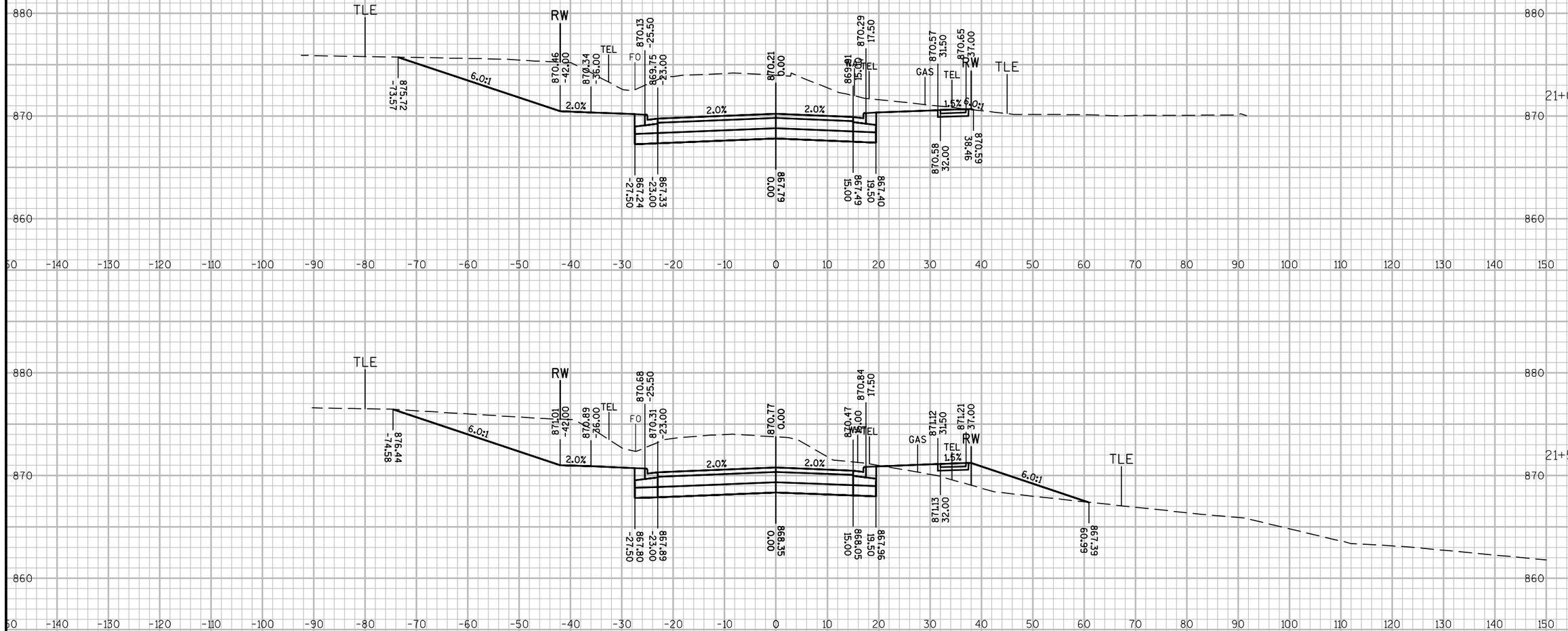
PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 140 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (14)
PLOT DATE : 7/21/2016 7:55 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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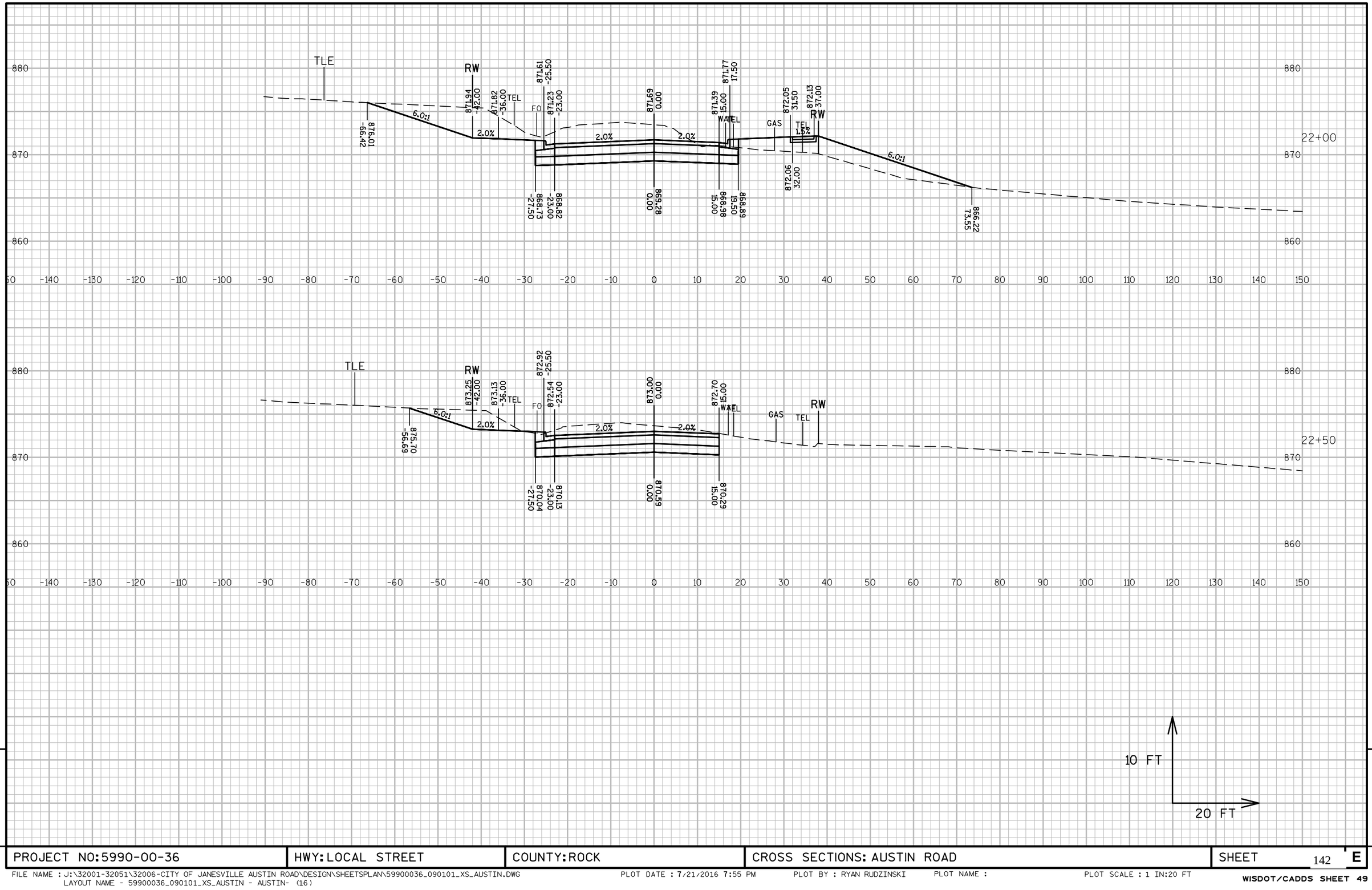
PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 141 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (15)
PLOT DATE : 7/21/2016 7:55 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49

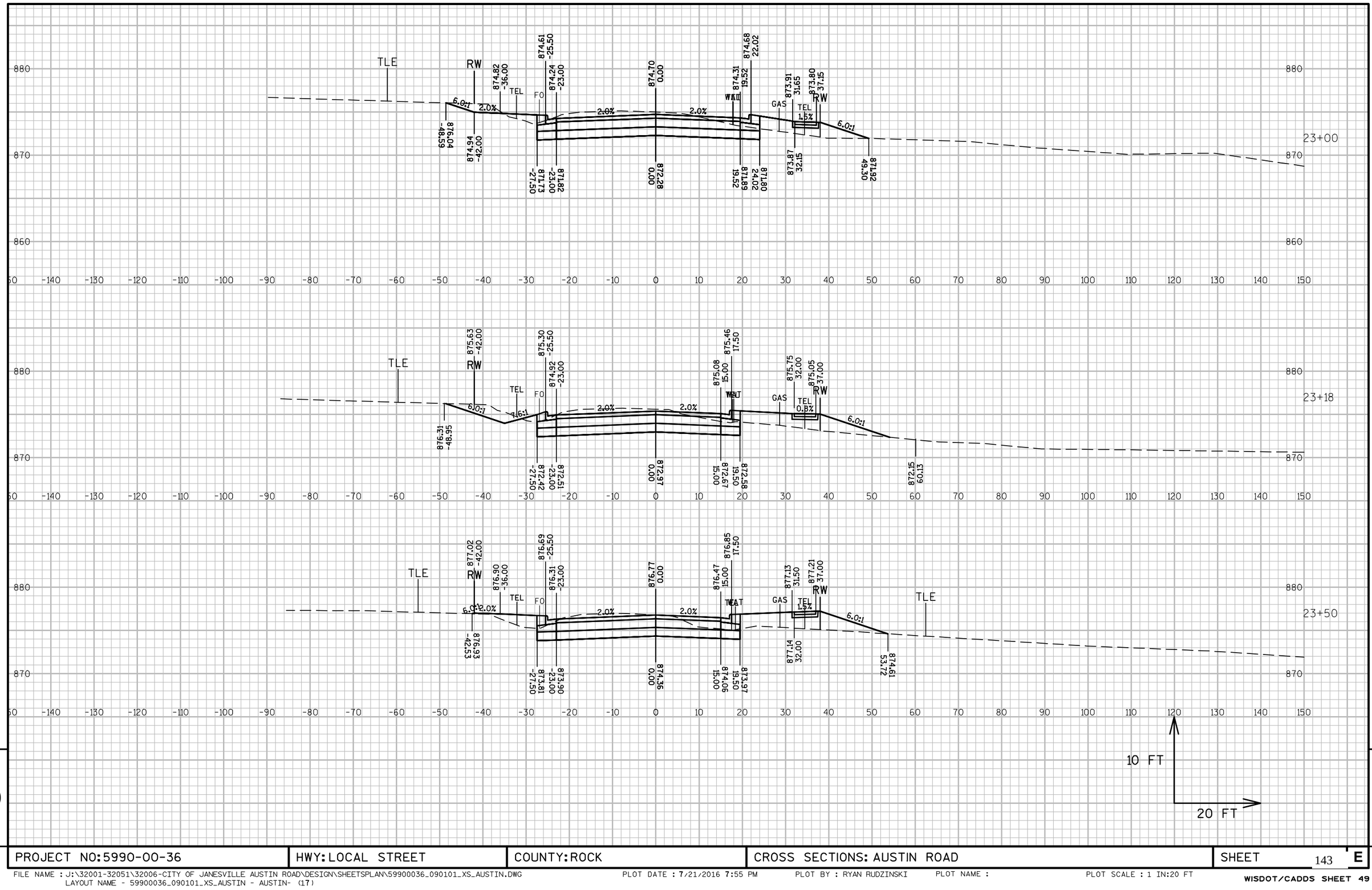


9

9

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 142 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (16)
PLOT DATE : 7/21/2016 7:55 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49

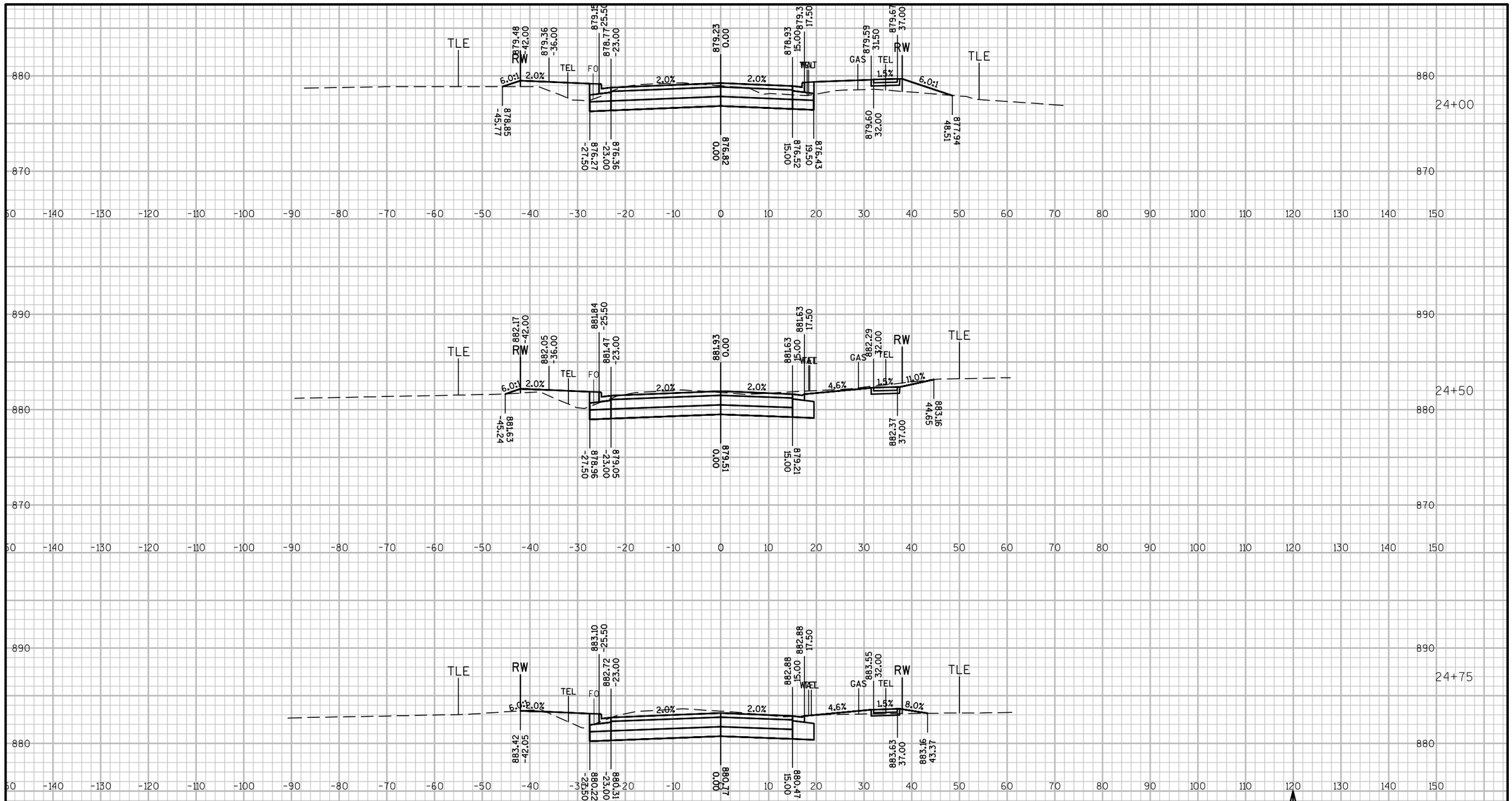


PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 143 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (17)

PLOT DATE : 7/21/2016 7:55 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT

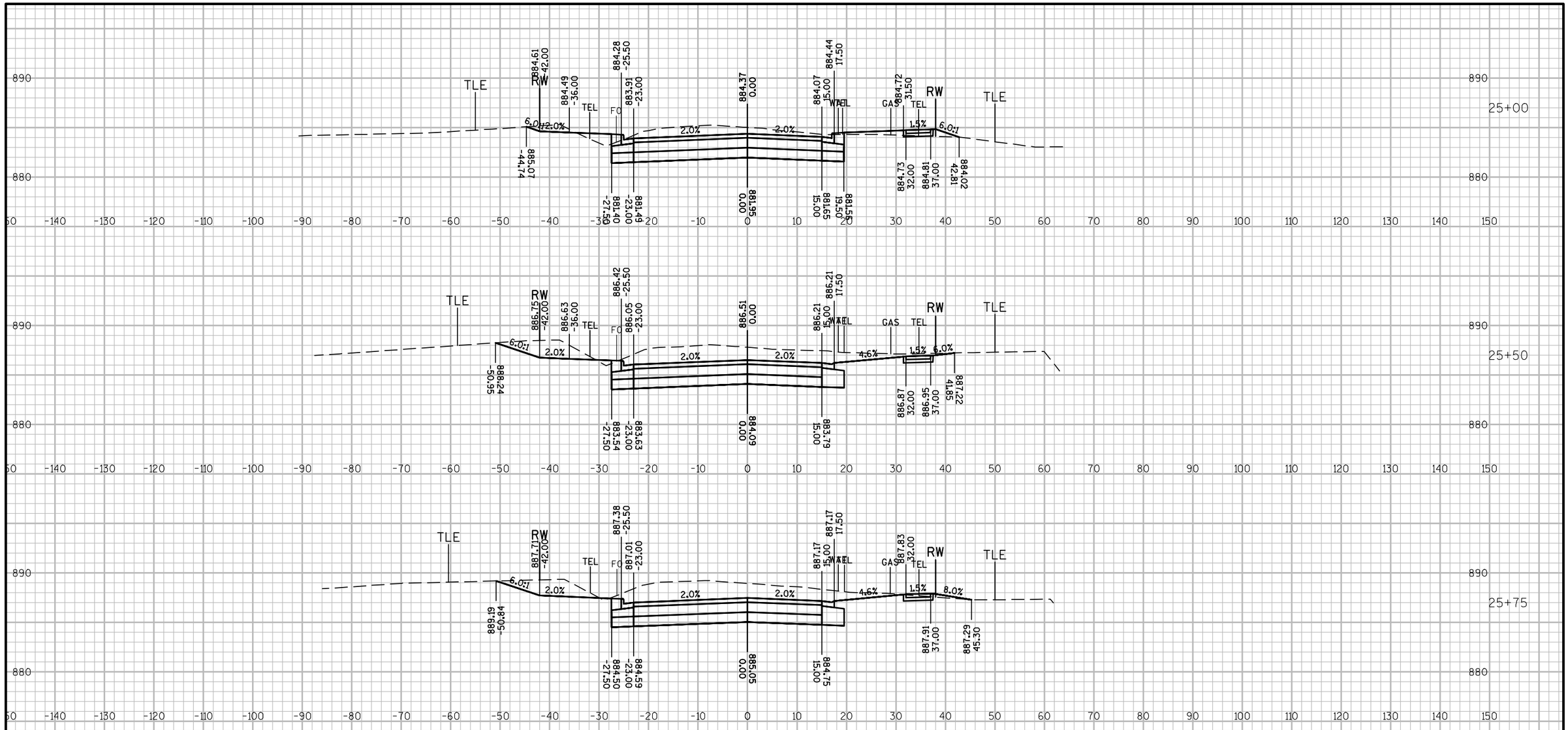
WISDOT/CADDS SHEET 49



PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 144 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (18)

PLOT DATE : 7/21/2016 7:56 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT



9

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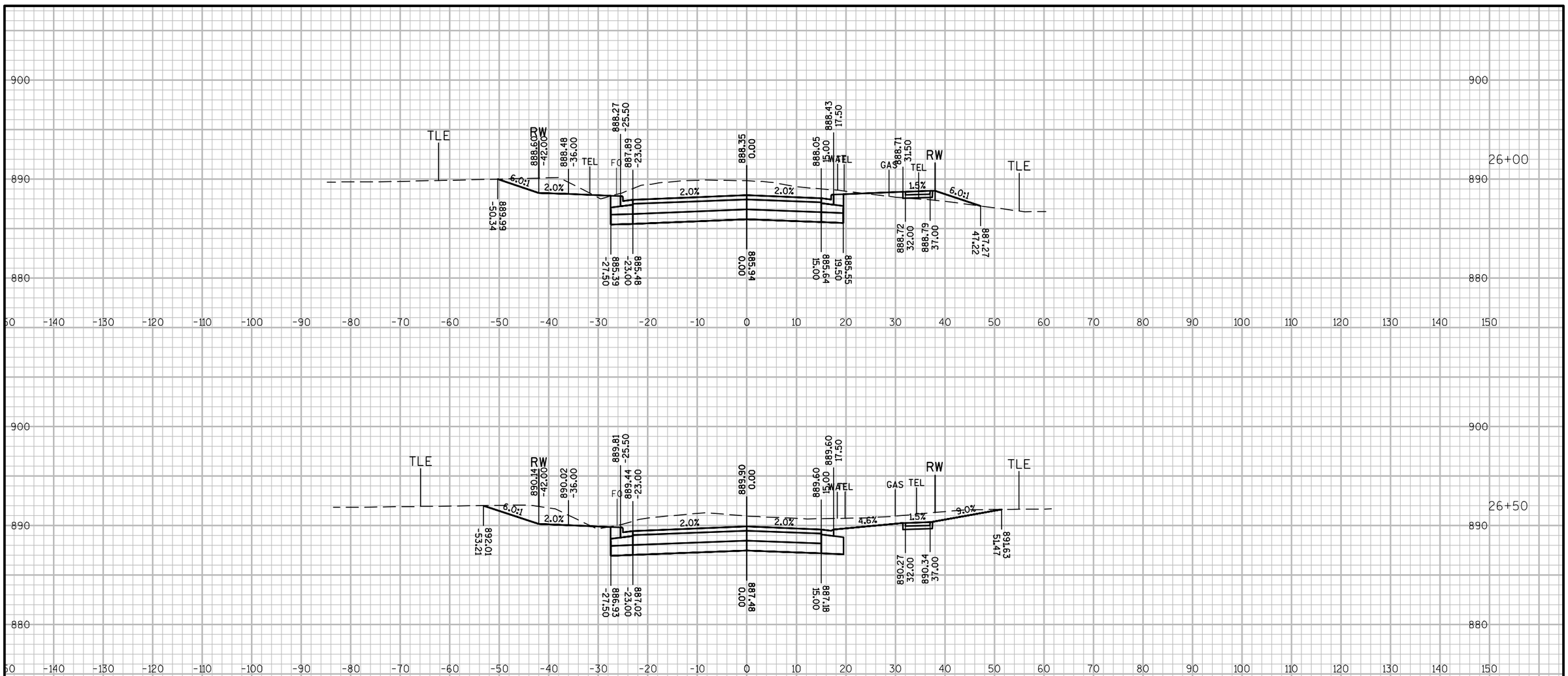
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 145 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (19)
PLOT DATE : 7/21/2016 7:56 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



9

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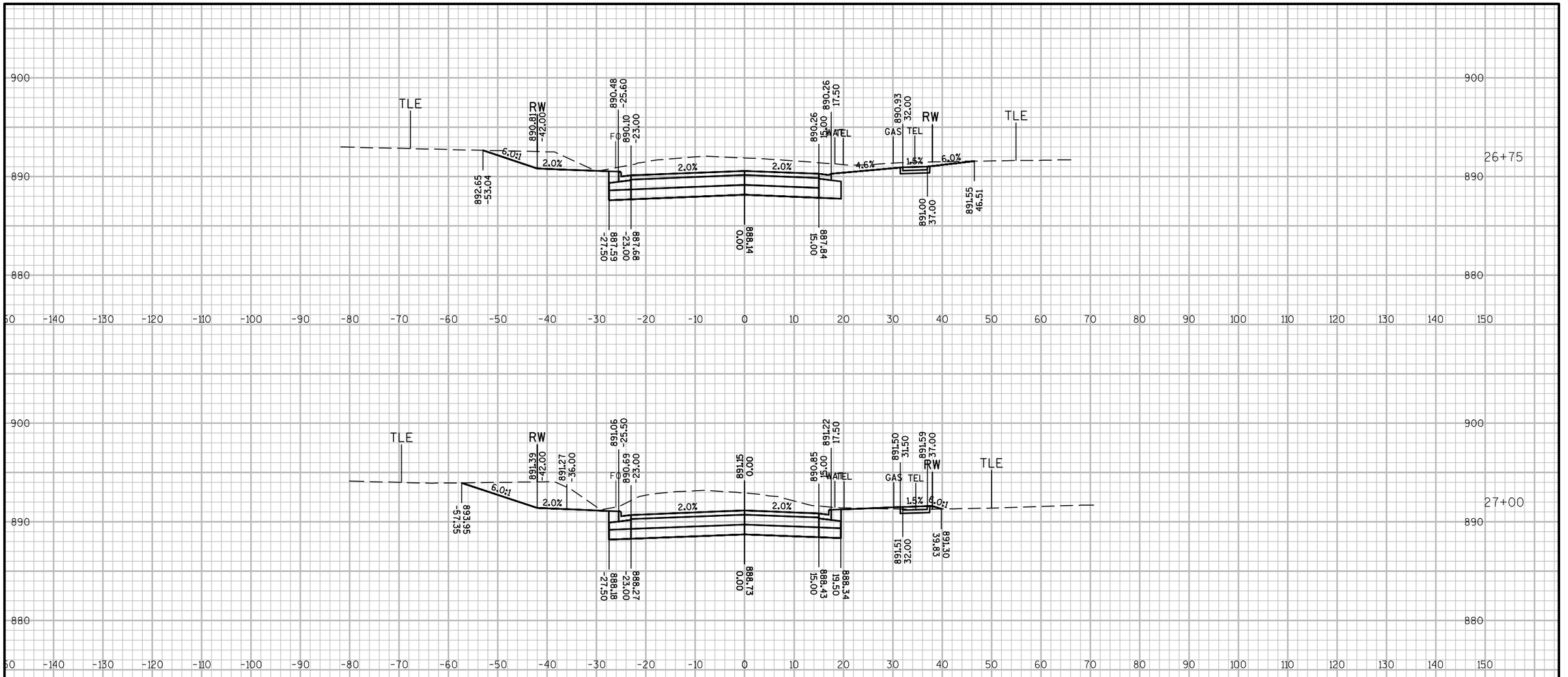
PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 146 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (20)

PLOT DATE : 7/21/2016 7:56 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 49



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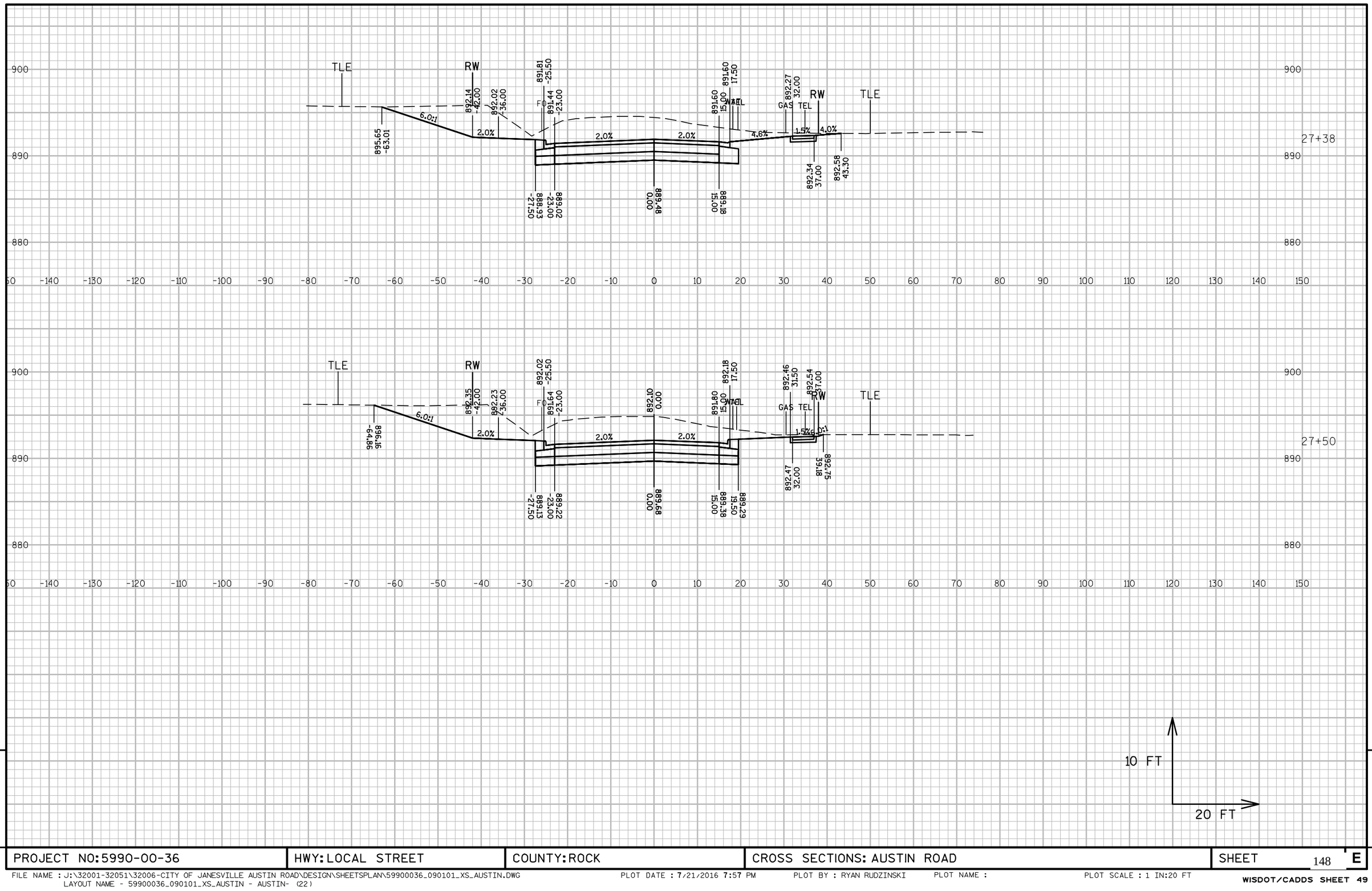
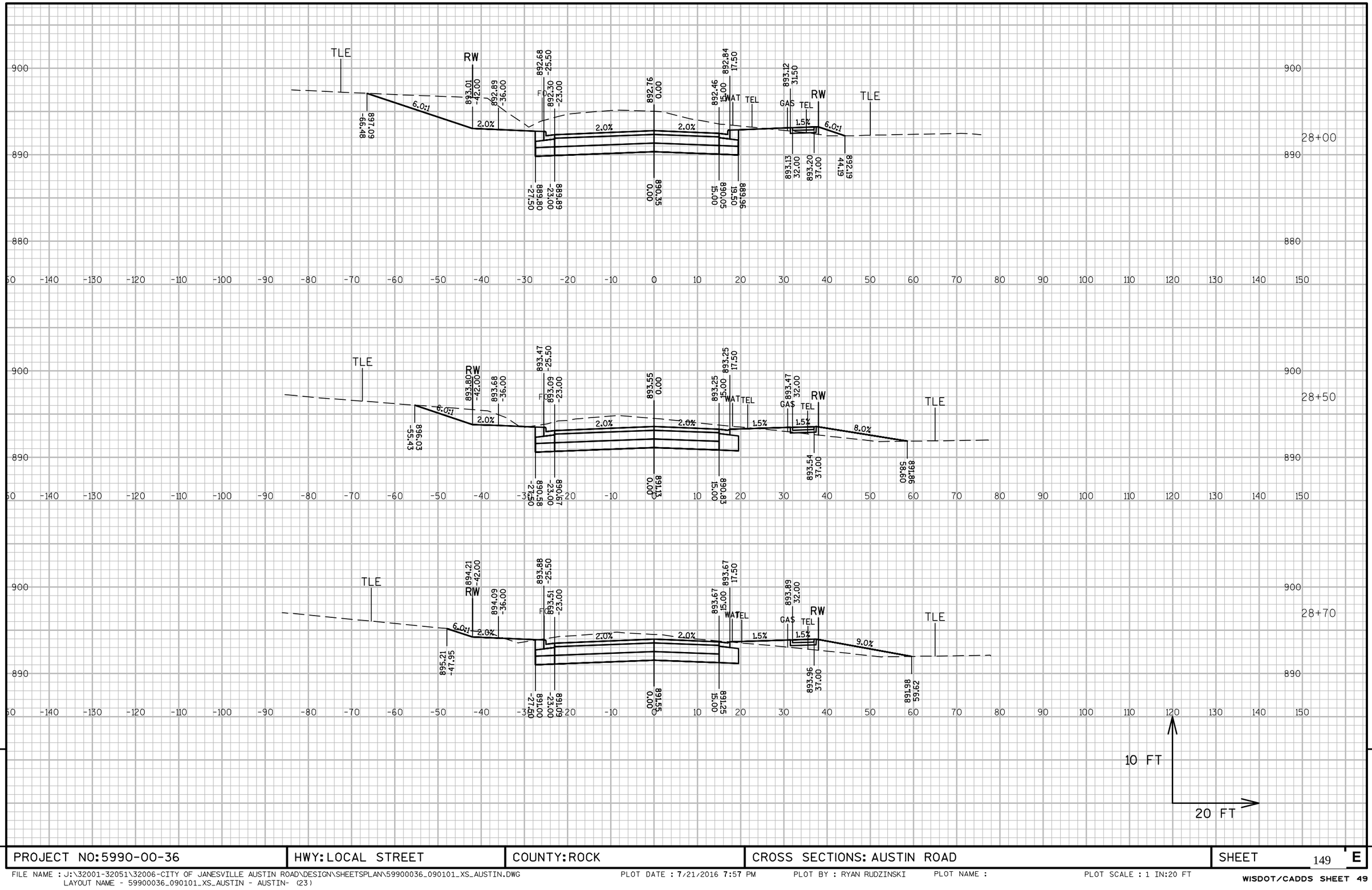
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 147 E

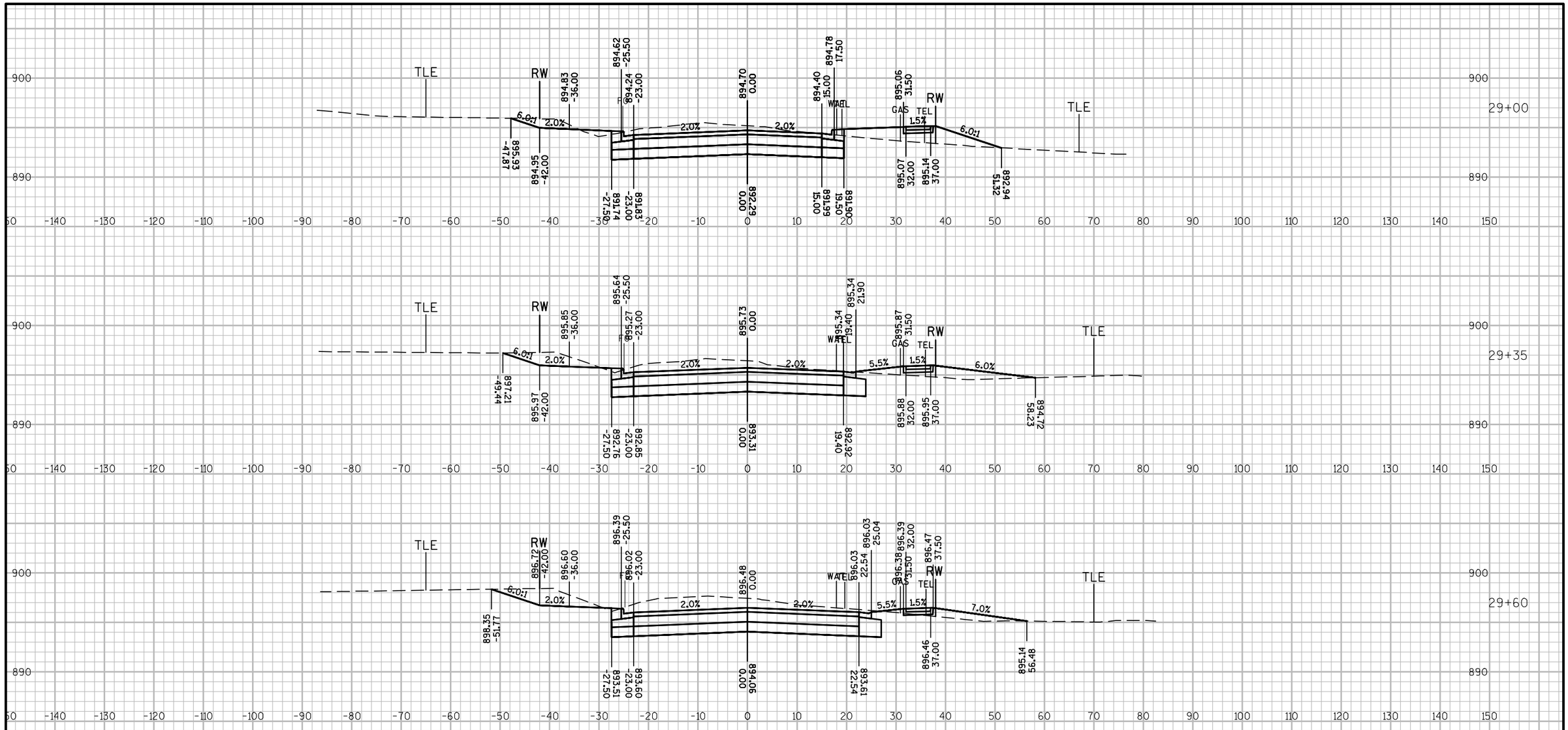


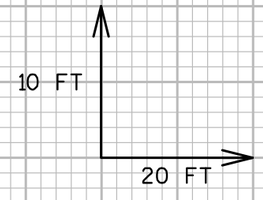
PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 148 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (22)
PLOT DATE : 7/21/2016 7:57 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 149 E





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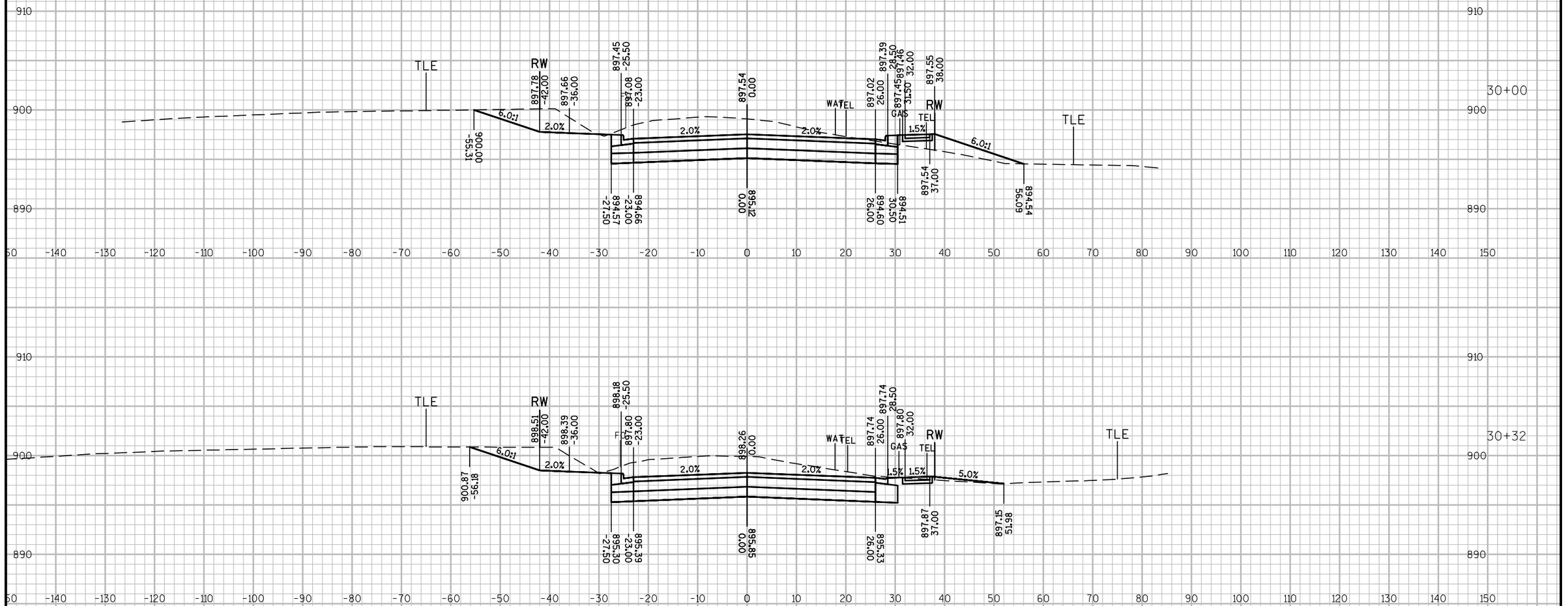
9

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 150 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (24)
PLOT DATE : 7/21/2016 7:57 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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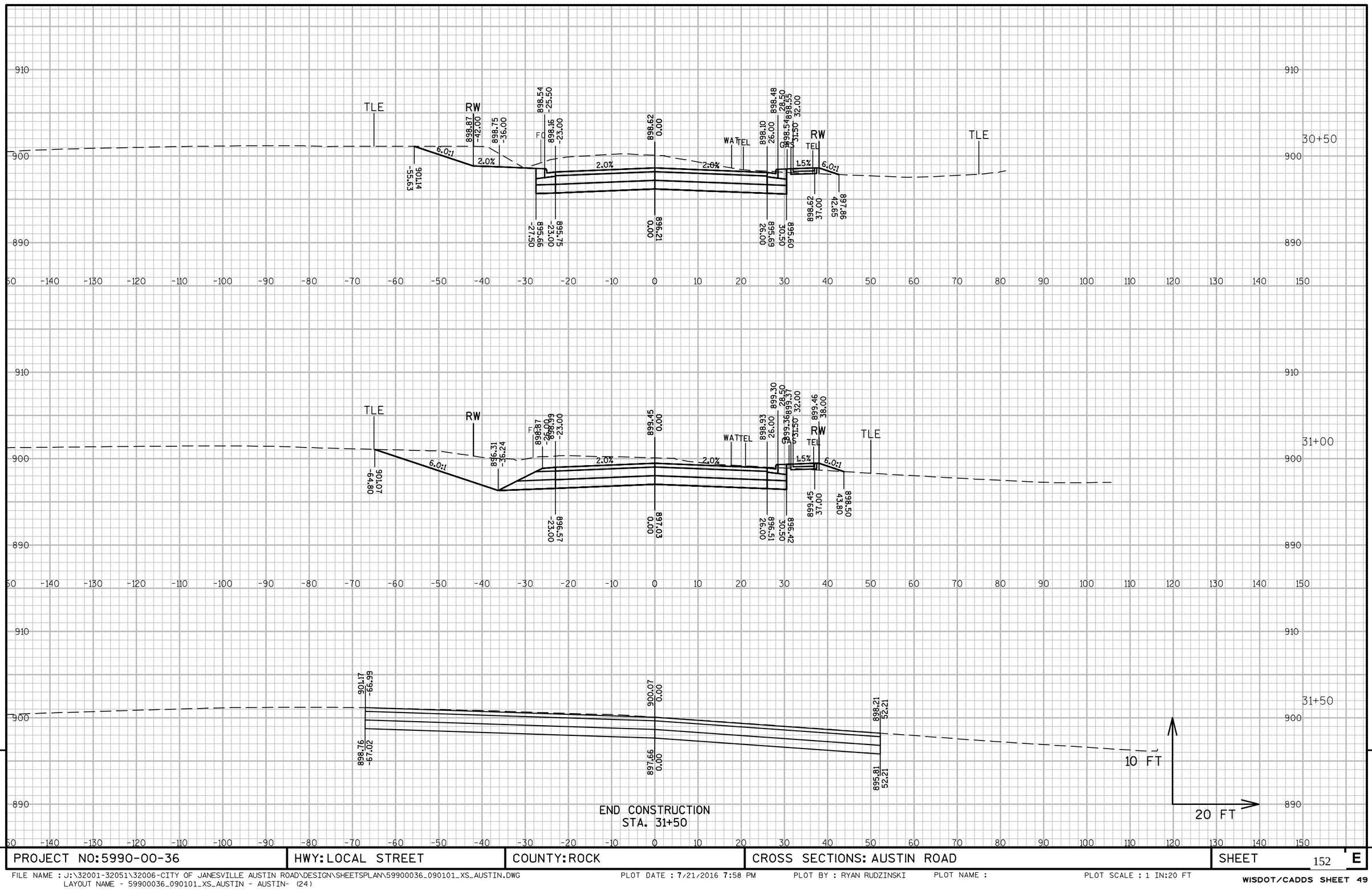
PROJECT NO:5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 151 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (24)

PLOT DATE : 7/21/2016 7:57 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 49

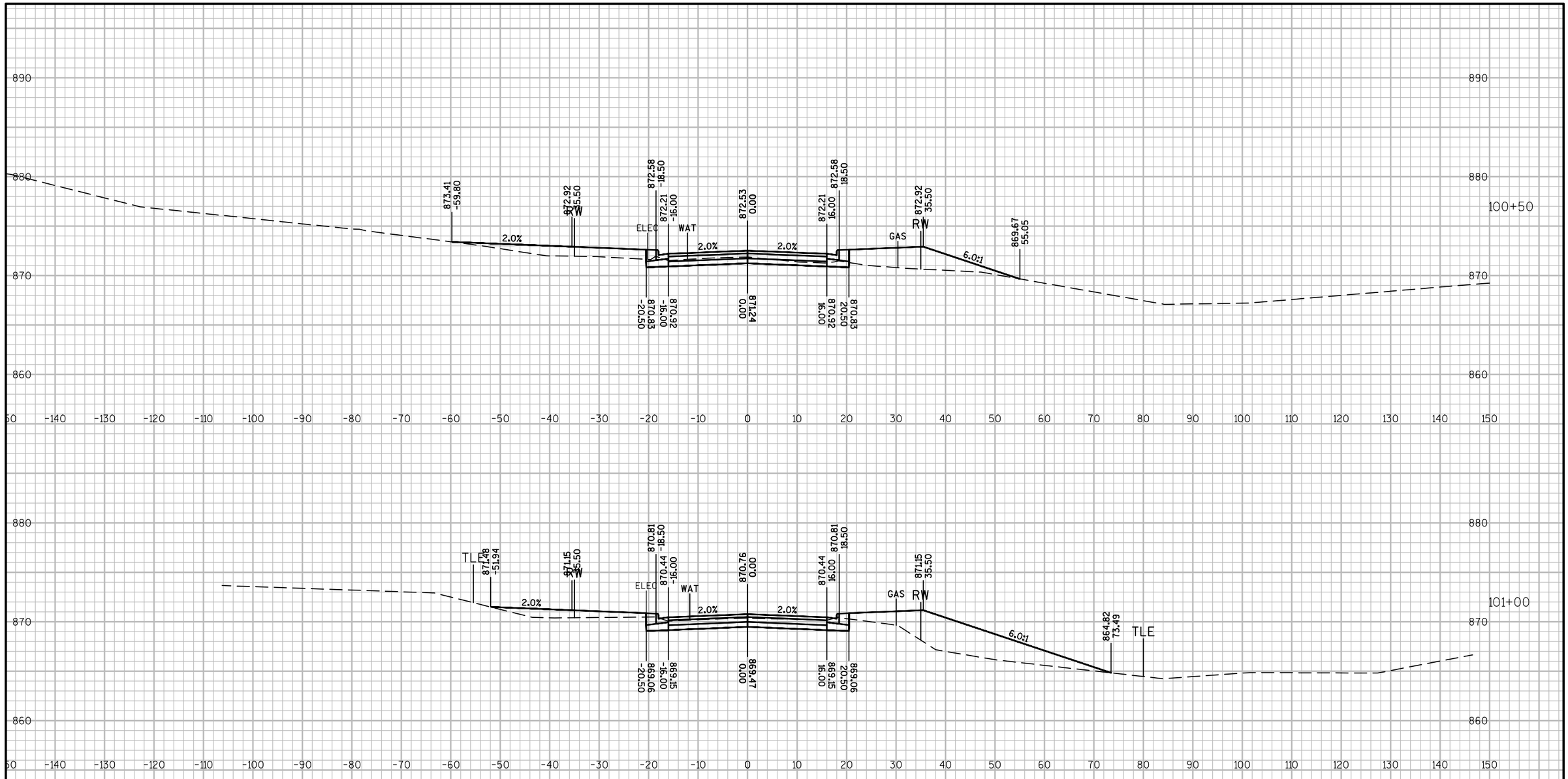


9

9

PROJECT NO:5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: AUSTIN ROAD	SHEET 152 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090101_XS_AUSTIN.DWG
LAYOUT NAME - 59900036_090101_XS_AUSTIN - AUSTIN- (24)
PLOT DATE : 7/21/2016 7:58 PM PLOT BY : RYAN RUDZINSKI PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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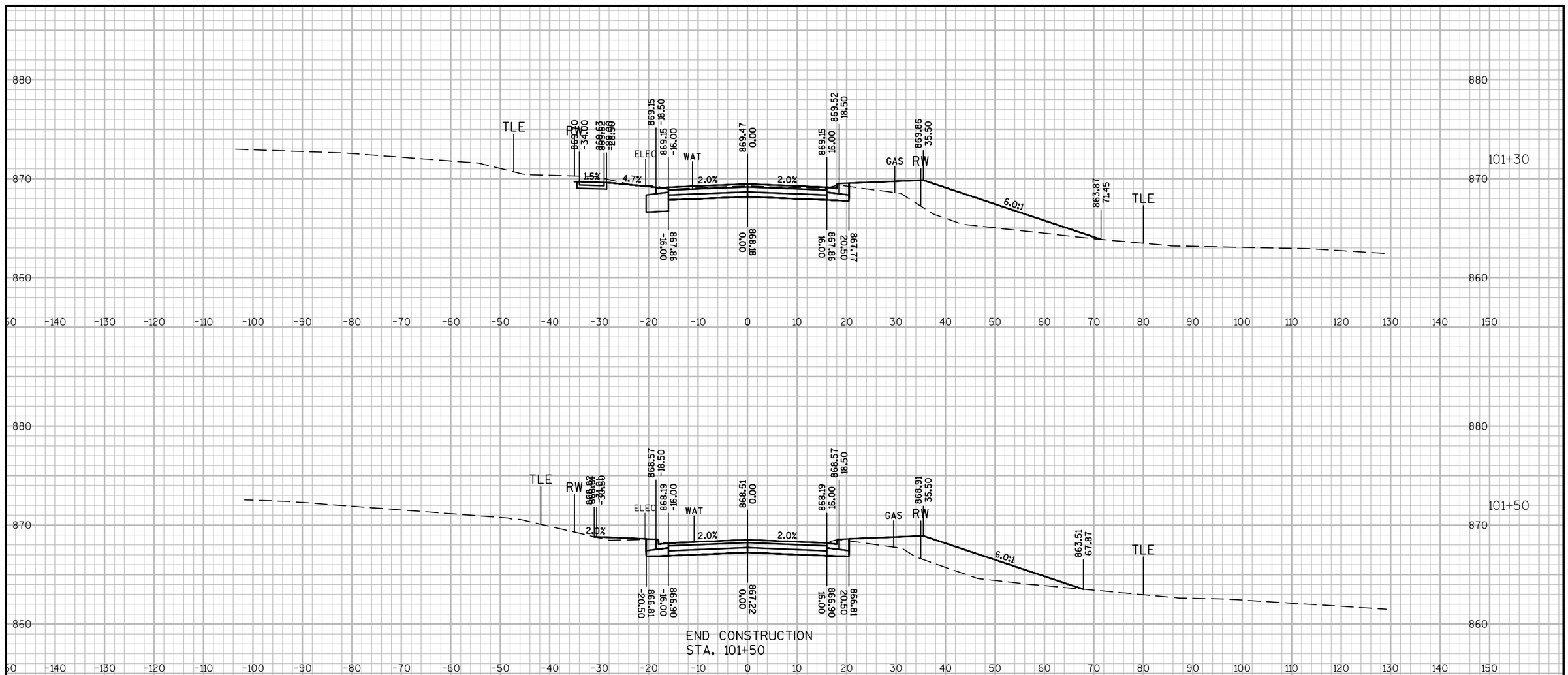
9

PROJECT NO: 5990-00-36	HWY: LOCAL STREET	COUNTY: ROCK	CROSS SECTIONS: KNOLLVIEW DRIVE	SHEET 153 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090102_XS_KNOLLVIEW.DWG
LAYOUT NAME - XS_KNOLLVIEW - (1)
PLOT DATE : 7/21/2016 4:35 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 49



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PROJECT NO:5990-00-36	HWY:LOCAL STREET	COUNTY:ROCK	CROSS SECTIONS: KNOLLVIEW DRIVE	SHEET 154 E

FILE NAME : J:\32001-32051\32006-CITY OF JANESVILLE AUSTIN ROAD\DESIGN\SHEETSPLAN\59900036_090102_XS_KNOLLVIEW.DWG
LAYOUT NAME - XS_KNOLLVIEW - (2)

PLOT DATE : 7/21/2016 4:35 PM PLOT BY : ALEXANDER FEULING PLOT NAME : PLOT SCALE : 1 IN:20 FT

Notes

Wisconsin Department of Transportation

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