DEC 2016 FEDERAL PROJECT STATE PROJECT STATE OF WISCONSIN PROJECT CONTRACT ORDER OF SHEETS PROJECT WITH: 5100-08-72 WISC 2016466 Section No. 1 DEPARTMENT OF TRANSPORTATION Section No. 2 Typical Sections and Details Estimate of Quantities Section No. 3 Miscellaneous Quantities Section No. 3 ₽ PLAN OF PROPOSED IMPROVEMENT Section No. 4 Right of Way Plat Plan and Profile Section No. 5 Standard Detail Drawings Section No. 6 CASHTON - HILLSBORO Section No. 7 Sign Plates Section No. 8 Structure Plans Section No. 9 Computer Earthwork Data STH 27 TO VILLAGE OF CASHTON EVL 0-08-Section No. 9 Cross Sections STH 33 TOTAL SHEETS = 374 **MONROE COUNTY** STATE PROJECT NUMBER 5100-08-72 DESIGN DESIGNATION A.A.D.T. 2011 = 4500 A.A.D.T. 2037 = 5900 D.H.V. = 5.4 BEGIN PROJECT 5100-08-72 D.D. = 60/40 STA 61+86.95 = 6.5% X = 631163.27DESIGN SPEED = 35 MPH TOWNSHIP OF VILLAGE OF CASHTON Y = 305889.72**ESALS** = 1,300,000 CONVENTIONAL SYMBOLS 10NR0 END PROJECT 5100-08-72 PROFILE PI AN STA 116+00.31 GRADE LINE CORPORATE LIMITS ORIGINAL GROUND PROPERTY LINE MARSH OR ROCK PROFILE LOT LINE (To be noted as such) LIMITED HIGHWAY EASEMENT SPECIAL DITCH EXISTING RIGHT OF WAY STATE OF WISCONSIN GRADE ELEVATION PROPOSED OR NEW R/W LINE DEPARTMENT OF TRANSPORTATION CULVERT (Profile View) SLOPE INTERCEPT PREPARED BY UTILITIES WISDOT REFERENCE LINE Surveyor ELECTRIC TRAVIS MIKSHOWSKY Designer EXISTING CULVERT FIBER OPTIC ANTHONY VANDERWIELEN Project Manager PROPOSED CULVERT GAS SANITARY SEWER JOSEPH GREGAS Regional Supervisor COMBUSTIBLE FLUIDS LAYOUT STORM SEWER 1 MILE TELEPHONE SCALE L MARSH AREA HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY TATE: 7-25-2016 Int M. Vadaliliede, COORDINATES, MONROE COUNTY, NAD83 (2007), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID UTILITY PEDESTAL TOTAL NET LENGTH OF CENTERLINE = 1.025 MILES POWER POLE DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES. THE ELEVATIONS ARE NAVD88 (2007). WOODED OR SHRUB AREA TELEPHONE POLE FILE NAME: S:\MAD\1000--1099\1089\837\ACAD\CIVIL 3D\SHEETSPLAN\010101_TI.DWG PLOT DATE: 7/8/2016 9:49 AM PLOT BY : JACK, ROBERT A. PLOT NAME : WISDOT/CADDS SHEET 10

UTILITY CONTACTS

DAVE BEKKUM CASHTON MUN ELECTRIC AND WATER UTIL-ELECTRICITY P.O. BOX 188 CASHTON, WI 54619 (608) 654-5160 cashtondpw@villageofcashton.org

CENTURYLINK-COMMUNICATION LINE 311 S. COURT STREET SPARTA, WI 54656 (608) 487-0637 bret.clark@centurylink.com

TIM ORCUTT MEDIACOM WISCONSIN LLC -COMMUNICATION LINE 3033 ASBURY RD DUBUQUE, IA 52001 (515) 249-5848 torcuttemediacomcc.com

LATROY BRUMFIELD WE ENERGIES-GAS/PETROLEUM 333 WEST EVERETT ST, ROOM A299 MILWAUKEE, WI 53203 (414) 221-5617 latroy.brumfield@we-energies.com

TRAVIS KAHL WE ENERGIES - GAS/PETROLEUM 1921 8TH STREET SOUTH WISCONSIN RAPIDS, WI 54494 (715) 421-7256 travis.kahl@we-energies.com

DAVE BEKKUM CASHTON MUNICIPAL - SEWER P.O. BOX 188 CASHTON, WI 54619 (608) 654-5160 cashtondpw@villageofcashton.org

TODD TUNKS VERNON COMMUNICATIONS COOPERATIVE - COMMUNICATION LINE 103 N MAIN ST WESTBY, WI 54667 (608) 634-3136 ttunks@vernoncom.coop

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS, AS SHOWN ON THE PLANS, ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

WHEN PORTIONS OF EXISTING ASPHALTIC SURFACES ARE TO BE REMOVED TO ACCOMMODATE NEW CONSTRUCTION, THE LINE OF SUCH REMOVAL SHALL BE NEATLY DELINEATED WITH A SAW CUT JOINING THROUGH THE ASPHALTIC SURFACE SO THAT REMOVAL OF THE ASPHALT SHALL BE ACCOMPLISHED WITHOUT DAMAGE TO REMAINING PORTIONS. THE LOCATION OF SAW JOINTS AND THE AMOUNT REMOVED AT SIDE ROADS WILL BE DETERMINED IN THE FIELD BY

THE EXACT LOCATION OF PRIVATE AND FIELD ENTRANCES ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES EXCEPT WHEN PIPE LAYING OPERATIONS REQUIRE THE DRIVEWAY TO BE CLOSED. ACCESS TO DRIVEWAY SHALL BE RE-ESTABLISHED IMMEDIATELY AFTER PIPE IN DRIVEWAY AREA IS INSTALLED, ACCESS SHALL BE PROVIDED DURING ALL NON-WORKING HOURS.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDNAL JOINTS FROM BEING LOCATED WITHIN A DRIVING. TURNING. OR PARKING LANE.

SAW CUTS SHALL BE MARKED OUT BY THE ENGINEER IN THE FIELD.

THE EROSION CONTROL ITEMS SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. THE ENGINEER SHALL CONFIRM THE LOCATION OF EROSION CONTROL ITEMS. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DETERMINE THE MEASURE IS NO LONGER NECESSARY, AT THAT TIME, THE CONTRACTOR SHALL REMOVE THE TEMPORARY EROSION ITEM INCIDENTAL TO THE COST OF THE RESPECTIVE BID ITEM.

DNR CONTACT

KAREN M. KALVELAGE ENVIRONMENTAL ANALYSIS AND REVIEW SPECIALIST WISCONSIN DEPT. OF NATURAL RESOURCES 3550 MORMON COULEE ROAD LA CROSSE, WI 54601 PHONE: (608) 785-9115 karen.kalvelages@wlsconsin.gov

DESIGN CONTACTS

TONY VANDER WIELEN PROJECT MANAGER WISDOT SW REGION PROJECT DEVELOPMENT 3550 MORMON COULEE RD. LA CROSSE, WI 54601 (608) 789-7878

TRAVIS MIKSHOWSKY PROJECT DESIGNER WISDOT SW REGION PROJECT DEVELOPMENT 3550 MORMON COULEE RD. LA CROSSE, WI 54601 (608) 789-5705



STANDARD ABBREVIATIONS

	<u> </u>	/ \ D D \ L \ \ 1 /	1110110
AC.	ACRE	MAX.	MAXIMUM
AGG.	AGGREGATE	MGAL	1000 GALLONS
ΔH	AHEAD	MIN.	MINIMUM
<	ANGLE	N.C.	NORMAL CROWN OR NO CHANGE
AE, AEW	APRON ENDWALL	N	NORTH
ASPH.	ASPHALTIC	NO.	NUMBER
A.D.T.	AVERAGE DAILY TRAFFIC	PAV'T	PAVEMENT
3.F.	BACK FACE	P.L.E.	PERMANENT LIMITED EASEMENT
3K.	BACK	P.C.	POINT OF CURVATURE
BEG.	BEGIN	P.I.	POINT OF INTERSECTION
3.M.	BENCH MARK	P.T.	POINT OF TANGENCY
C/L	CENTER LINE	V.P.C.	VERTICAL POINT OF CURVATURE
)	CENTRAL ANGLE OR DELTA	V.P.I.	VERTICAL POINT OF INTERSECTION
C.M.C.P.	CORRUGATED METAL CULVERT PIPE	V.P.T.	VERTICAL POINT OF TANGENCY
C.M.P.	CORRUGATED METAL PIPE	PCC	PORTLAND CEMENT CONCRETE
00.	COUNTY	P.E.	PRIVATE ENTRANCE
CTH	COUNTY TRUNK HIGHWAY	P.L.	PROPERTY LINE
CR.	CREEK	R	RADIUS OR RANGE
C.A.B.C.	CRUSHED AGGREGATE BASE COURSE	R/L	REFERENCE LINE
C.Y.	CUBIC YARD	R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
C.P.	CULVERT PIPE	RT	RIGHT
C & G	CURB AND GUTTER	REQ'D	REQUIRED
כ	DEGREE OF CURVE	R.H.F.	RIGHT HAND FORWARD
D.H.V.	DESIGN HOUR VOLUME	R/W	RIGHT OF WAY
DIA.	DIAMETER	R.	RIVER
DISCH.	DISCHARGE	RD.	ROAD
ĒΑ	EACH	SHLD.	SHOULDER(S)
.	EAST	SHR.	SHRINKAGE
ELEC.	ELECTRIC(AL), ELEC. CABLE	S	SOUTH
EL., ELEV.	ELEVATION	S.F.	SQUARE FOOT (FEET)
EXC.	EXCAVATION	SDD	STANDARD DETAIL DRAWING(S)
F.F.	FACE TO FACE	STH	STATE TRUNK HIGHWAY
ERT.	FERTILIZER	STA.	STATION
F.E.	FIELD ENTRANCE	S.E.	SUPERELEVATION
F/L, F.L.	FLOW LINE	S/L	SURVEY LINE
CWT.	HUNDRED WEIGHT	T	TANGENT
NL	INLET	TEL.	TELEPHONE
NTER.	INTERSECTION	TEMP.	TOP OF CURB
JT.	JOINT	T.L.E.	TANGENT
_T	LEFT	T.O.C.	TOP OF CURB
H.F.	LEFT HAND FORWARD	Т.	(TRUCKS) PERCENT OF
-•	LENGTH OF CURVE	TYP.	TYPICAL
_ . F.	LINEAR FOOT (FEET)	UNCL.	UNCLASSIFIED
_C.	LONG CHORD	U.G.	UNDERGROUND (CABLE)
_S	LUMP SUM	V.C.	VERTICAL CURVE
	MARKER ROCT	101	WEGT

SECTION 2 ORDER OF SHEETS

GENERAL NOTES PROJECT OVERVIEW TYPICAL SECTIONS CONSTRUCTION DETAILS PLAN DETAILS **EROSION CONTROL** STORM SEWER PERMANENT SIGNING AND PAVEMENT MARKING LIGHTING PLAN TRAFFIC CONTROL TYPICALS PCMS OVERVIEW PEDESTRIAN OVERVIEW TRAFFIC CONTROL STAGING ALIGNMENT DETAILS CONTROL POINT DATA

PROJECT NO:5100-08-72

HWY: STH 33

COUNTY: MONROE

PLOT DATE: 7/8/2016 10:05 AM

GENERAL NOTES

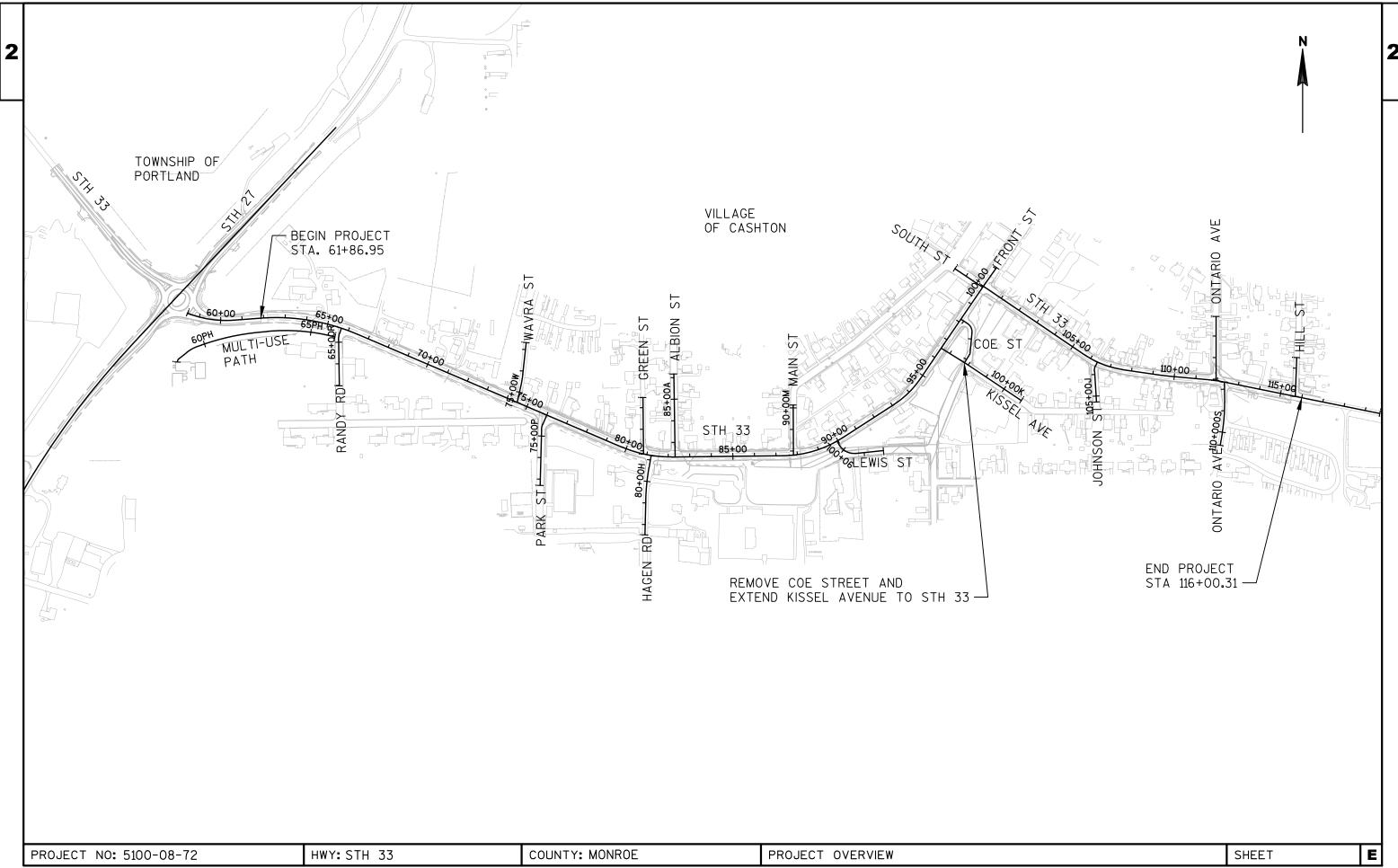
M.P.

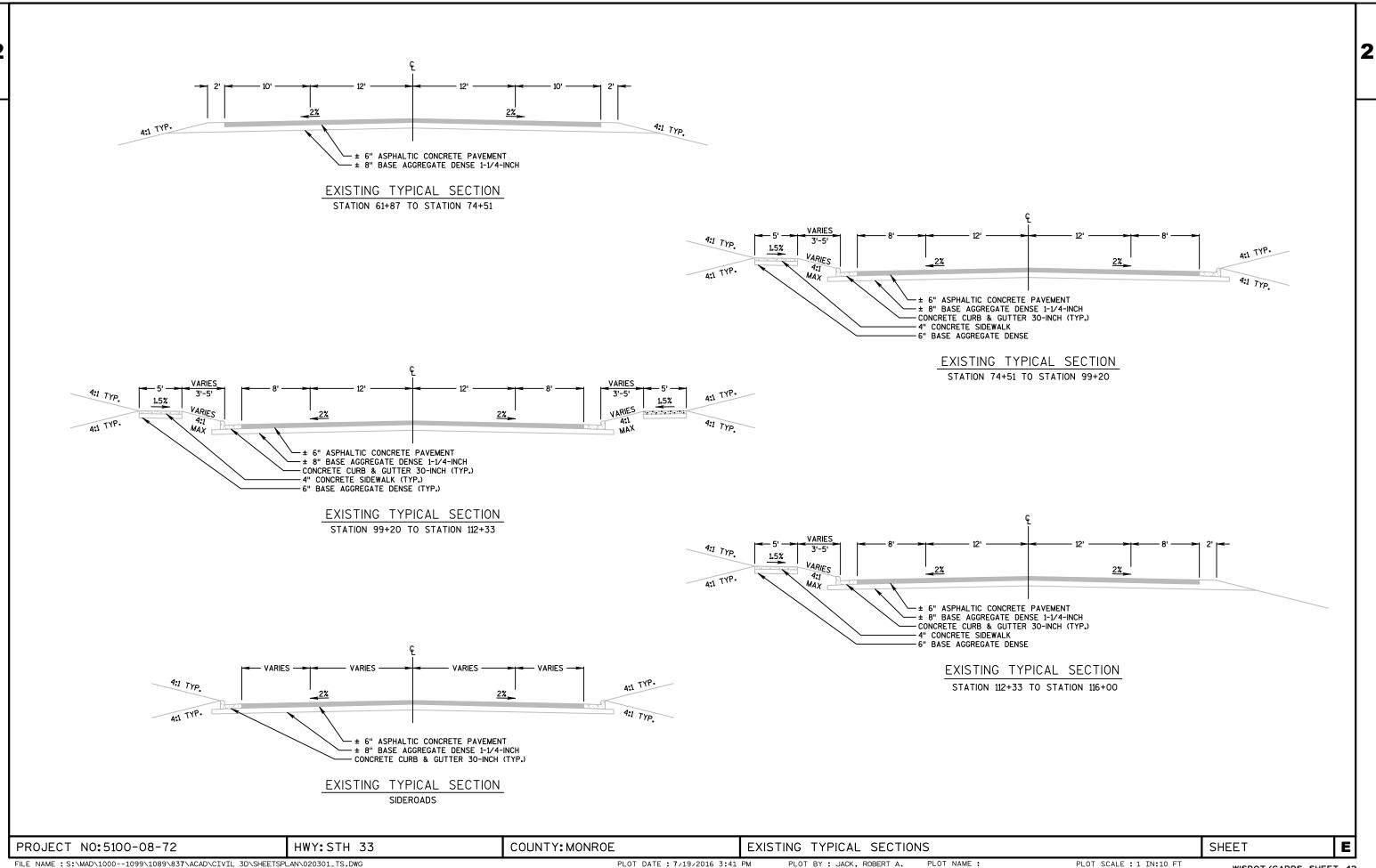
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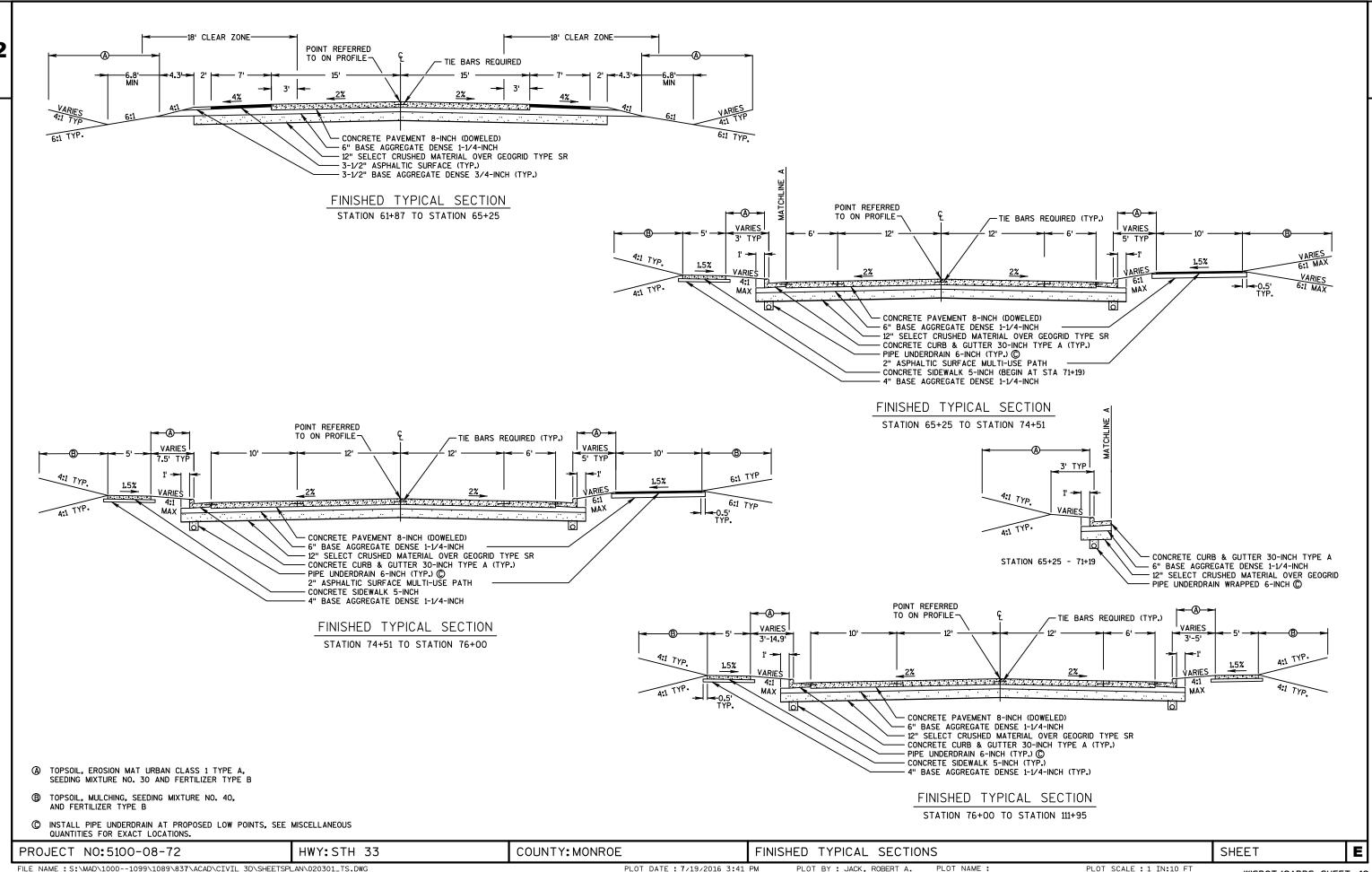
WEST

SHEET

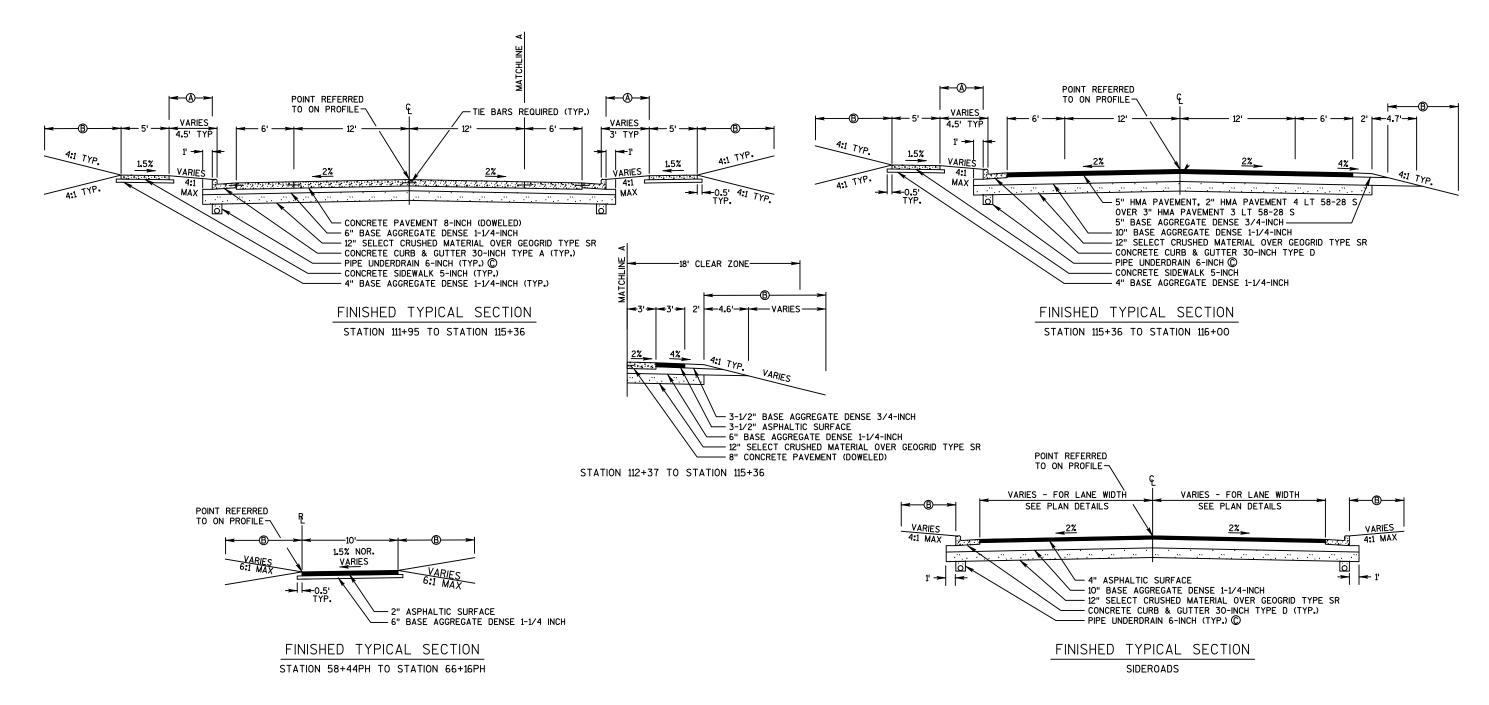
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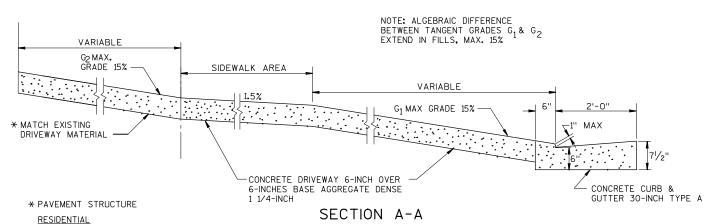






- A TOPSOIL, EROSION MAT URBAN CLASS 1 TYPE A, SEEDING MIXTURE NO. 30 AND FERTILIZER TYPE B
- B TOPSOIL, MULCHING, SEEDING MIXTURE NO. 40, AND FERTILIZER TYPE B
- $\ensuremath{\mathbb{C}}$ install pipe underdrain at proposed low points, see miscellaneous quantities for exact locations.

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE FINISHED TYPICAL SECTIONS SHEET **E**



ASPHALT - 2-INCHES ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES

OVER 6-INCHES BASE AGGREGATE DENSE 1 1/4-INCH

CONCRETE - CONCRETE DRIVEWAY 6-INCH OVER 6-INCHES BASE AGGREGATE DENSE 1 1/4-INCH

AGGREGATE - 6-INCHES BASE AGGREGATE DENSE 3/4-INCH

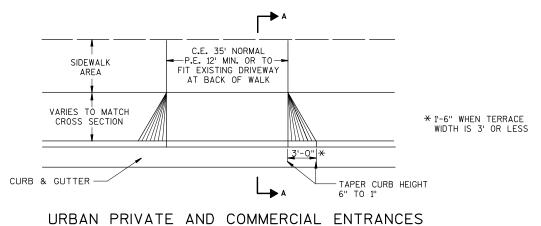
COMMERCIAL

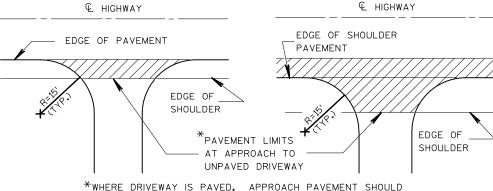
ASPHALT - 3-INCHES ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES OVER 8-INCHES BASE AGGREGATE DENSE 1 1/4-INCH

CONCRETE - CONCRETE DRIVEWAY 6-INCH OVER 6-INCHES BASE AGGREGATE

DENSE 1 1/4-INCH

AGGREGATE - 6-INCHES BASE AGGREGATE DENSE 3/4-INCH



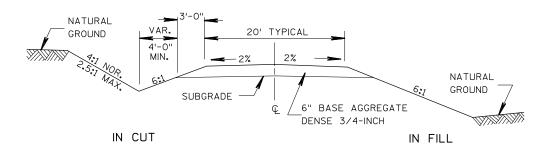


BE EXTENDED TO MATCH DRIVEWAY PAVEMENT.

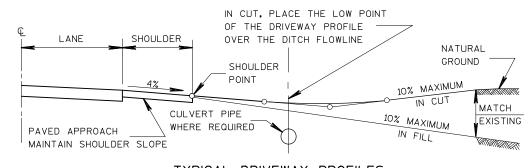
PLAN VIEW

PLAN VIEW (PAVED SHOULDER ON HIGHWAY)

RURAL DRIVEWAY INTERSECTION DETAIL

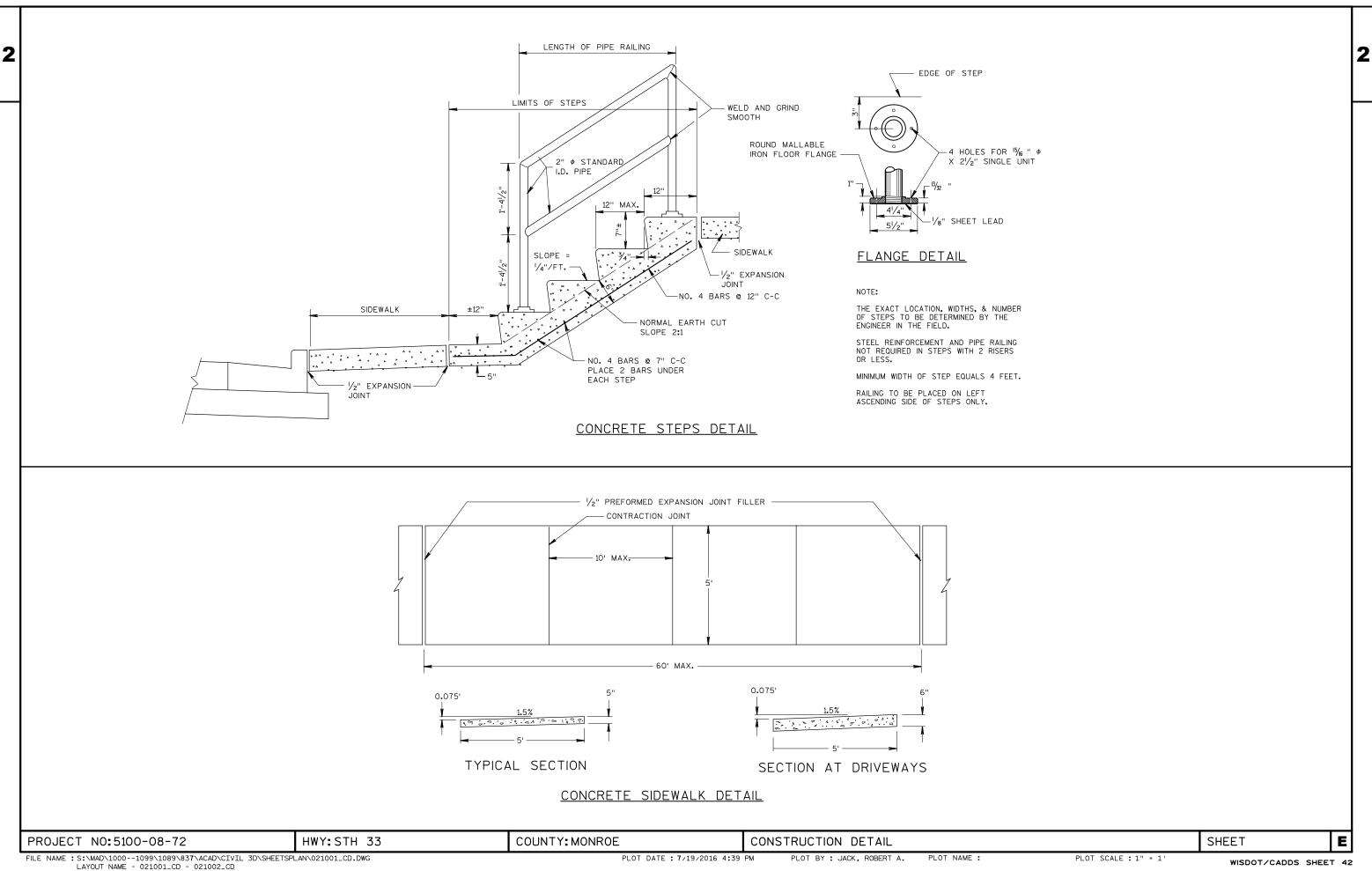


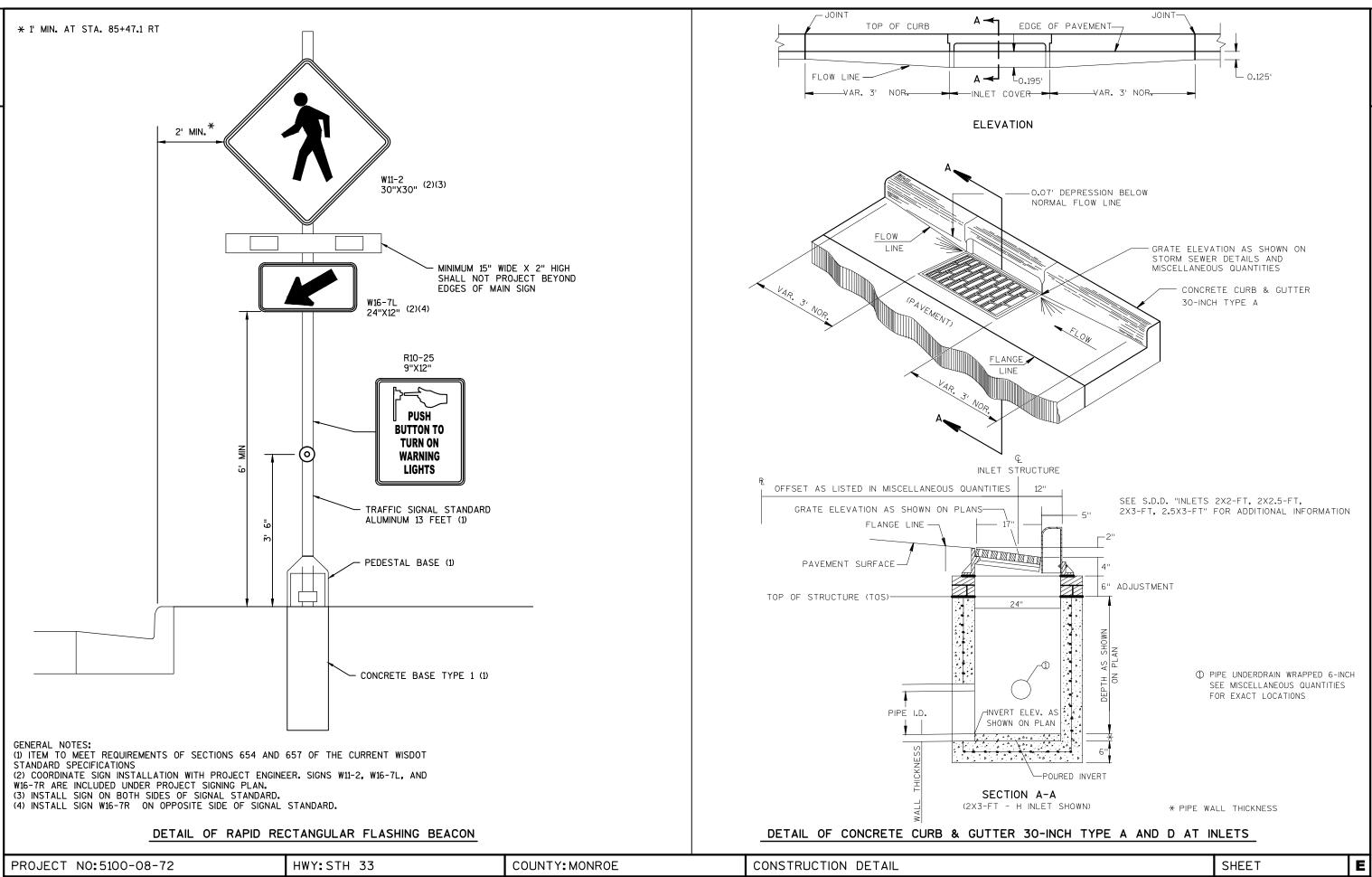
TYPICAL CROSS SECTION FOR PRIVATE DRIVE OR FIELD ENTRANCE

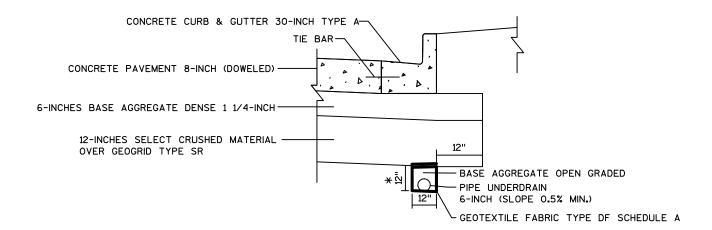


TYPICAL DRIVEWAY PROFILES

E PROJECT NO:5100-08-72 HWY:STH 33 COUNTY: MONROE CONSTRUCTION DETAIL SHEET



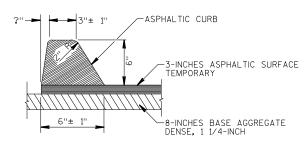




* DEPTH MAY VARY TO MAINTAIN 0.5% SLOPE

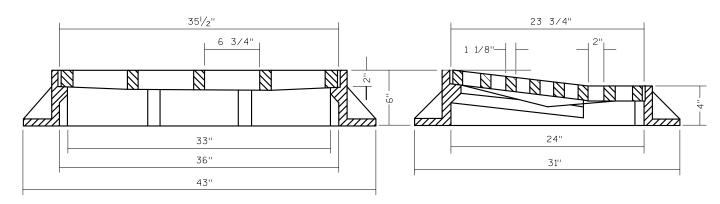
DETAIL FOR PIPE UNDERDRAIN 6-INCH WRAPPED

NOTE: PIPE UNDERDRAIN IS TO BE DISCHARGED INTO THE INLET STRUCTURES.



ASPHALTIC CURB DETAIL

TOTAL WEIGHT APPROXIMATELY 420 POUNDS



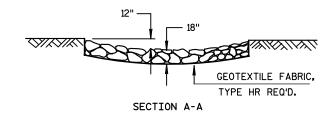
INLET COVERS TYPE DW

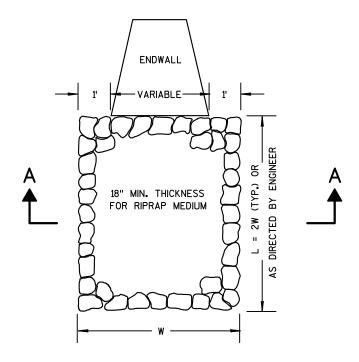
PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE CONSTRUCTION DETAIL

RUNOFF COEFFICIENT TABLE

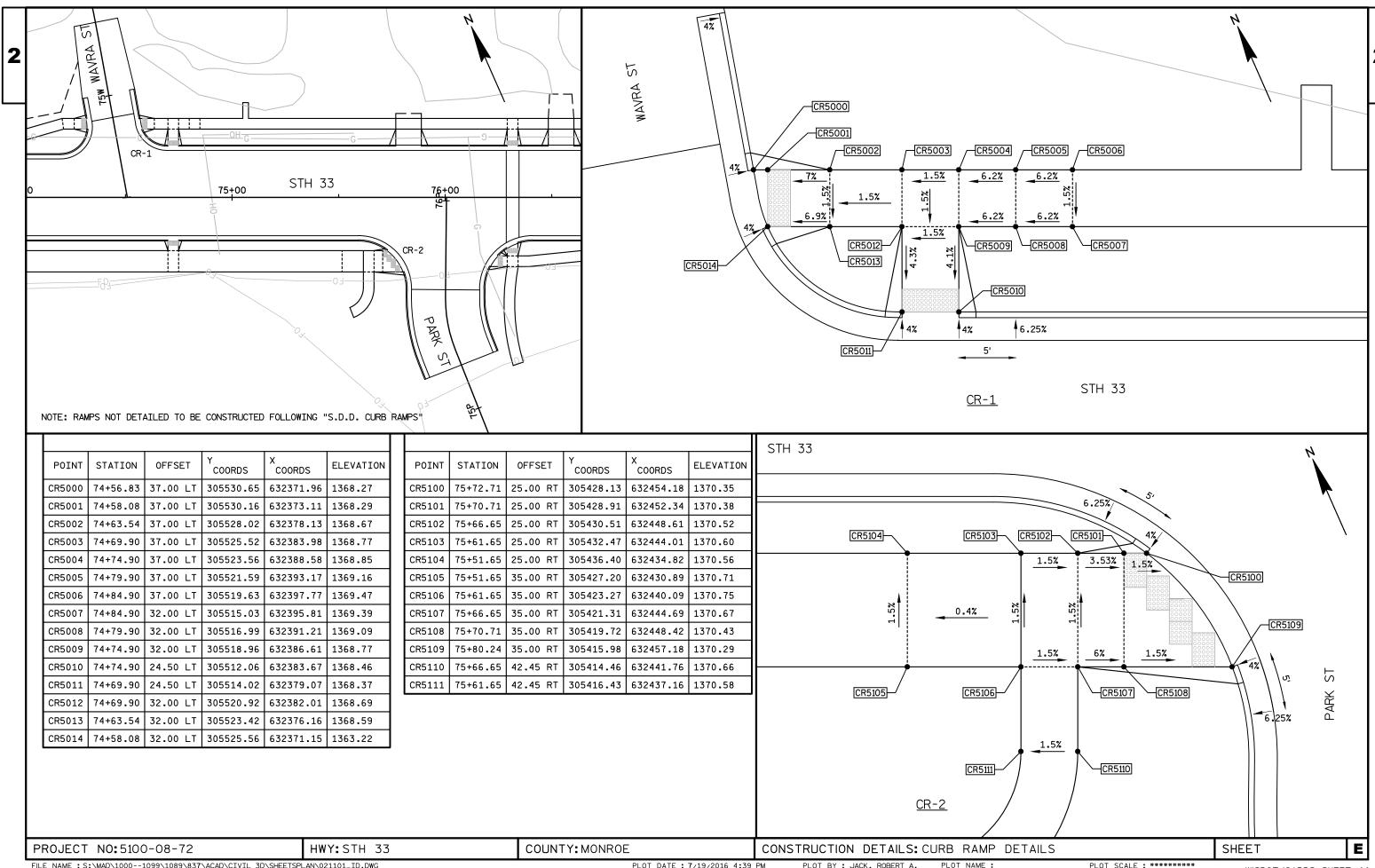
						HYDROLOGIC S	SOIL GROU	JP					
	A				Е	}		C	>		D		
	SLOPE	RANGE	(PERCENT)	SLOPE	RANGE	(PERCENT)	SLOPE	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	
ROW CROPS	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38	
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56	
MEDIAN STRIP-	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30	
TURF	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40	
SIDE SLOPE-			.25			.27			.28			.30	
TURF			.32			.34			.36			.38	
PAVEMENT:			•	•		•	•		•	•	ı	•	
ASPHALT						.7095							
CONCRETE						.8095							
BRICK						.7080							
DRIVES, WALKS						.7585							
R00FS						.7595							
GRAVEL ROADS,	SHOULDE	ERS				.4060							

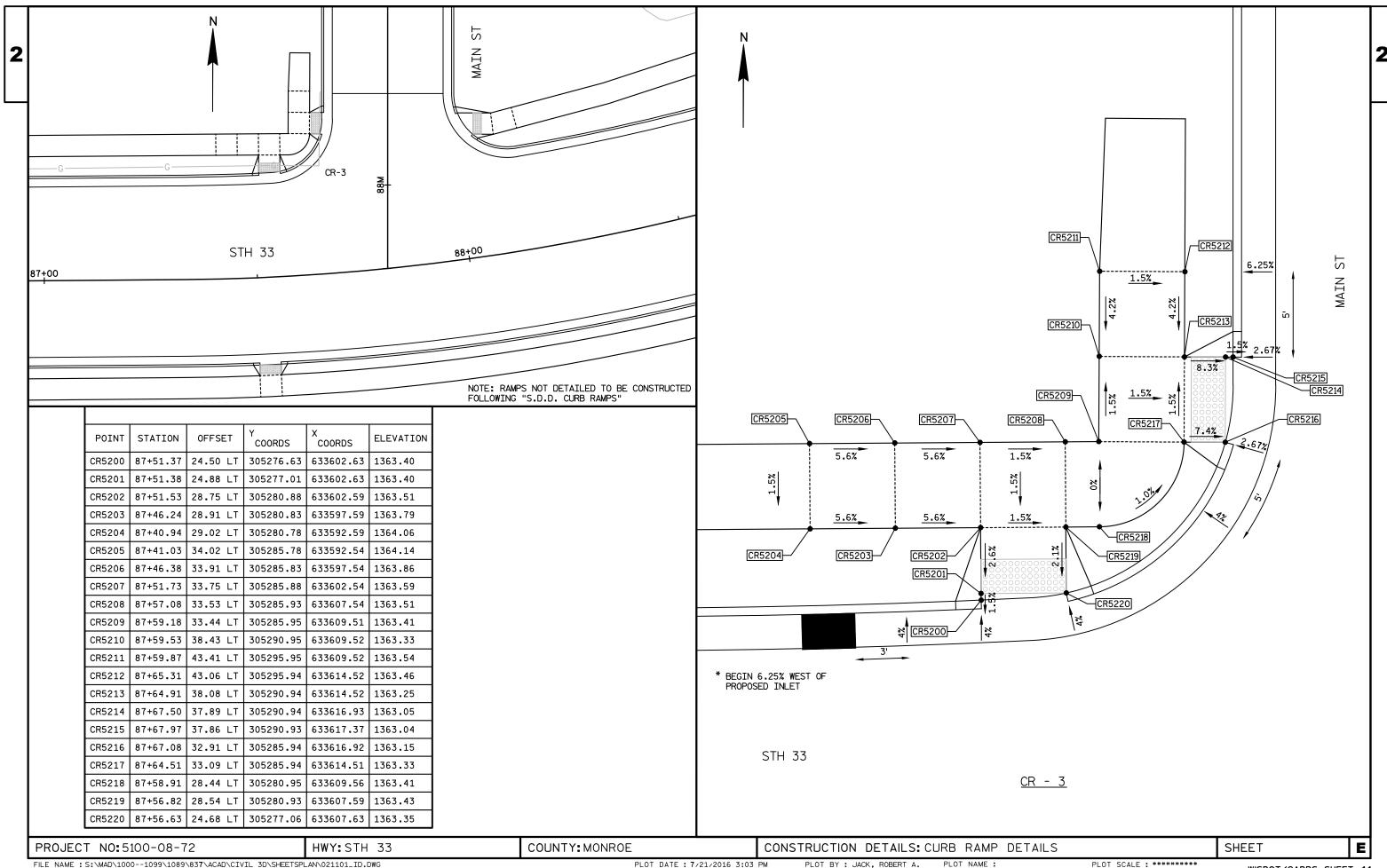
TOTAL PROJECT AREA = 13.82 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 10.66 ACRES

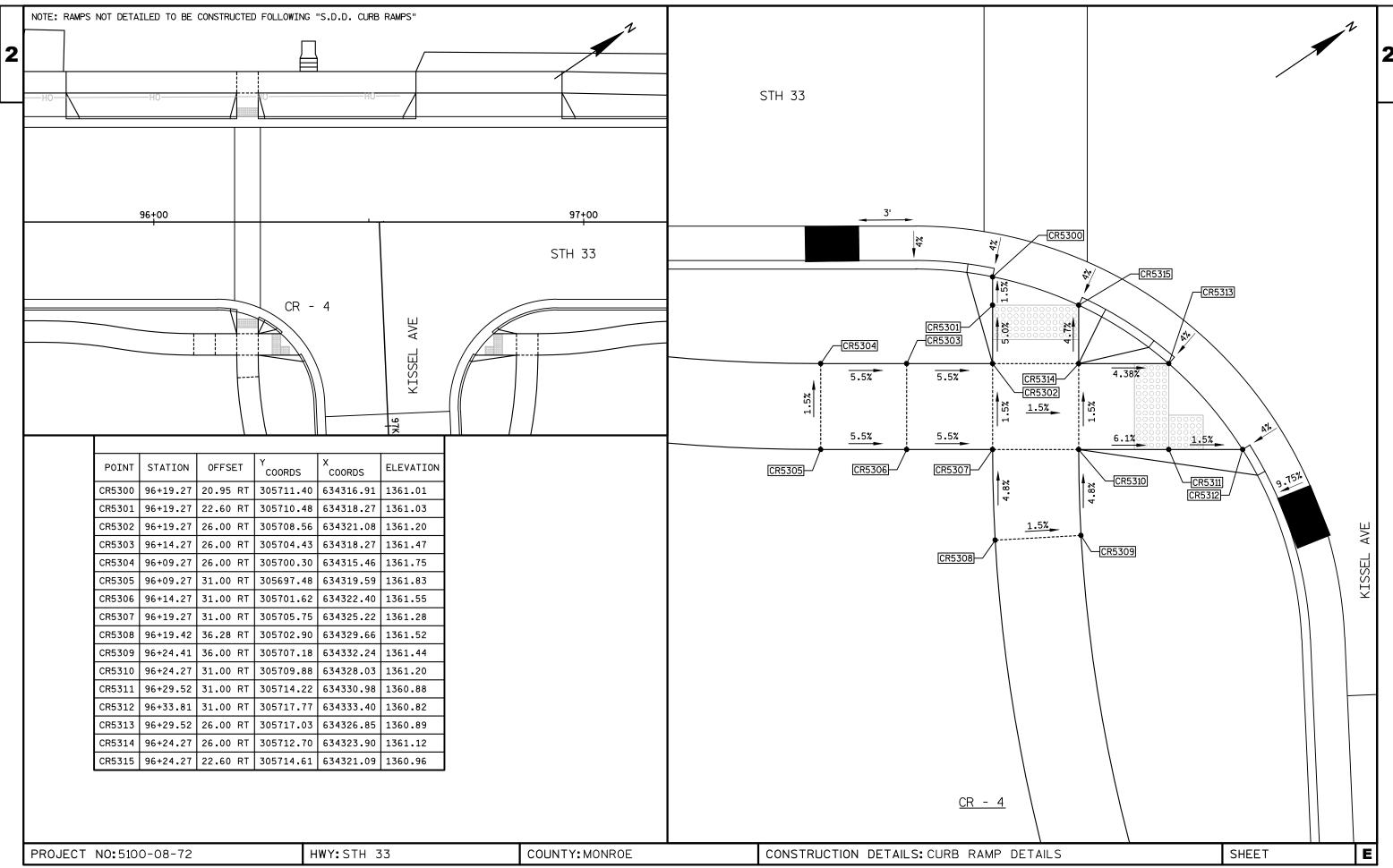


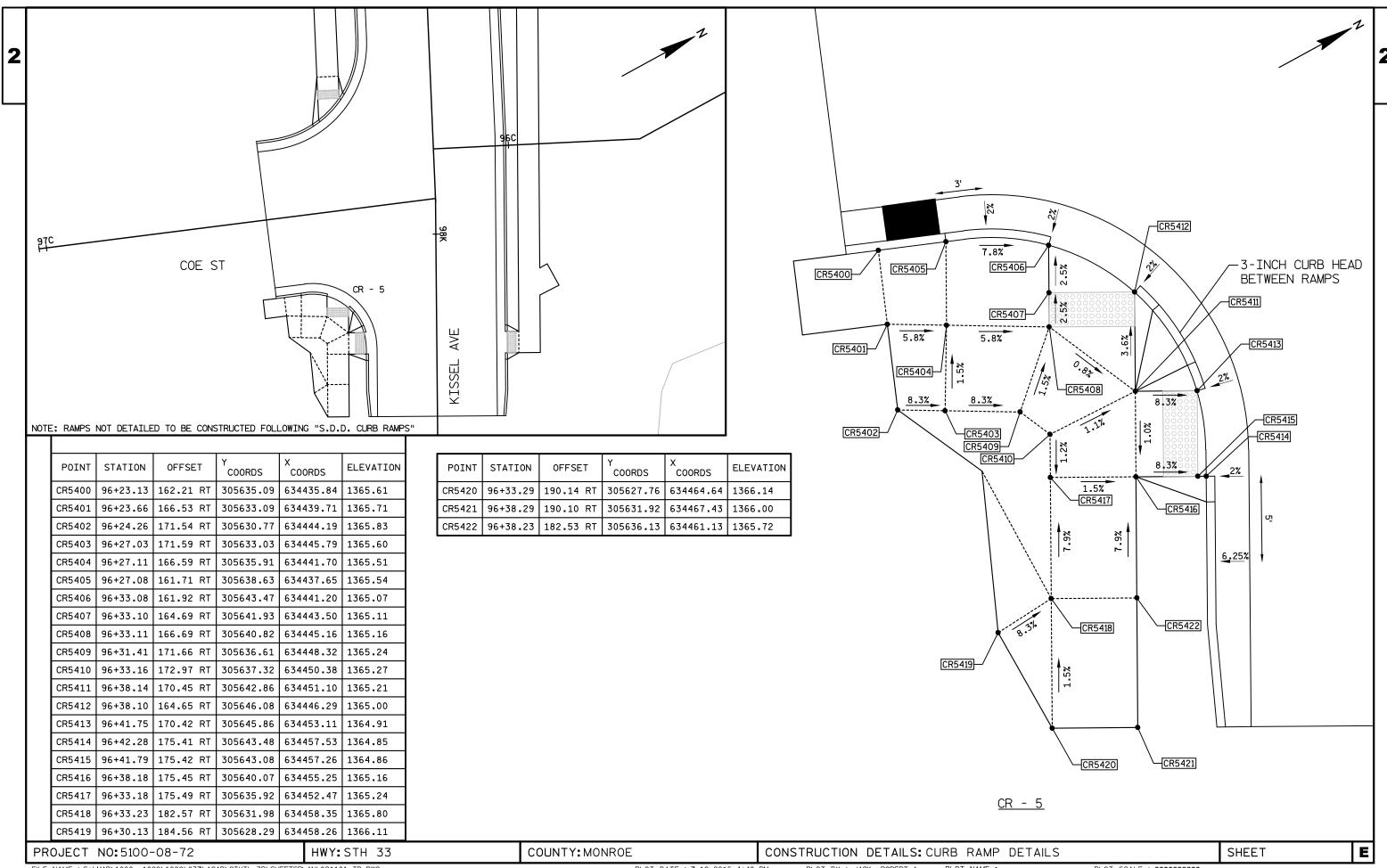


RIPRAP MEDIUM TREATMENT AT CULVERTS







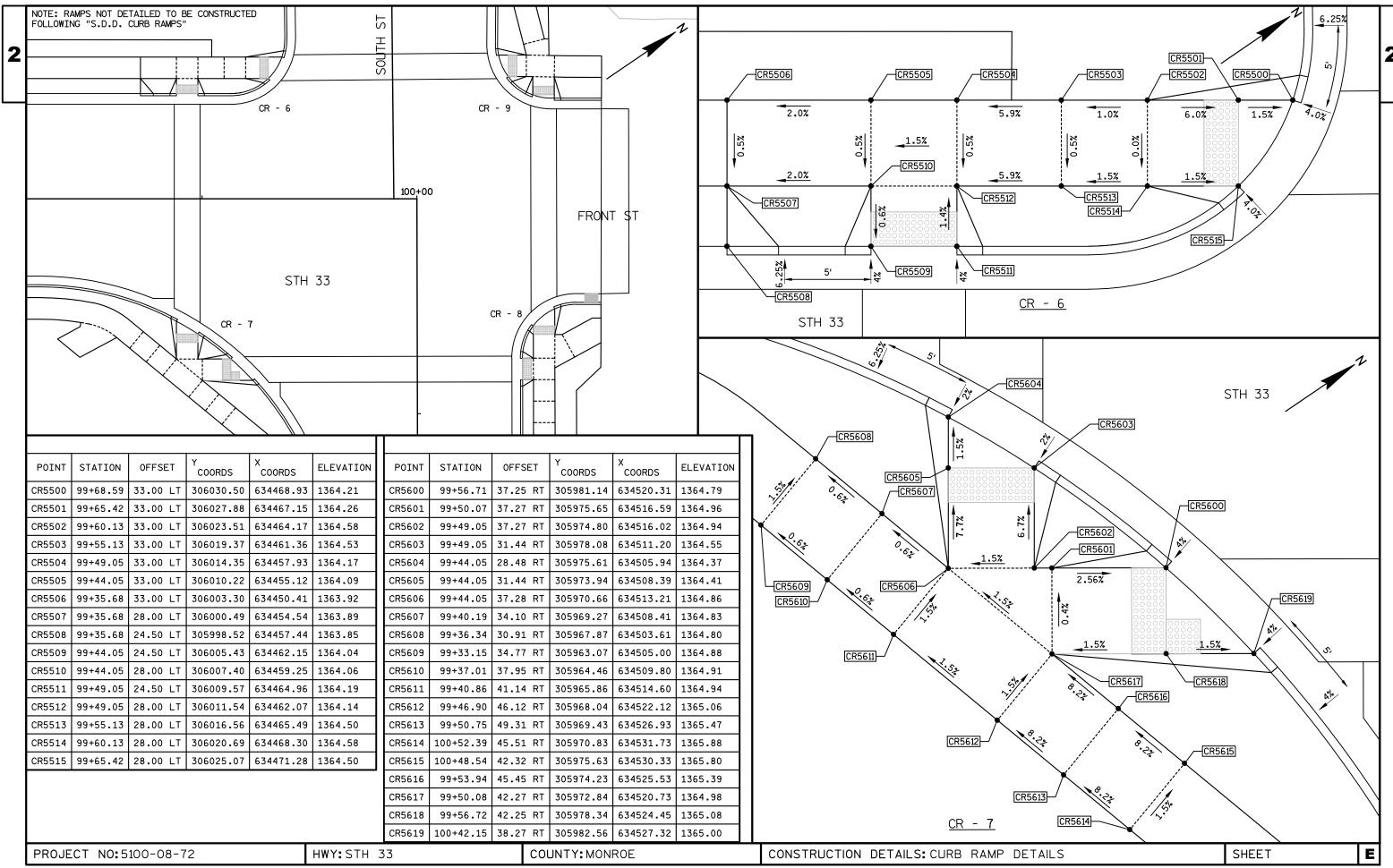


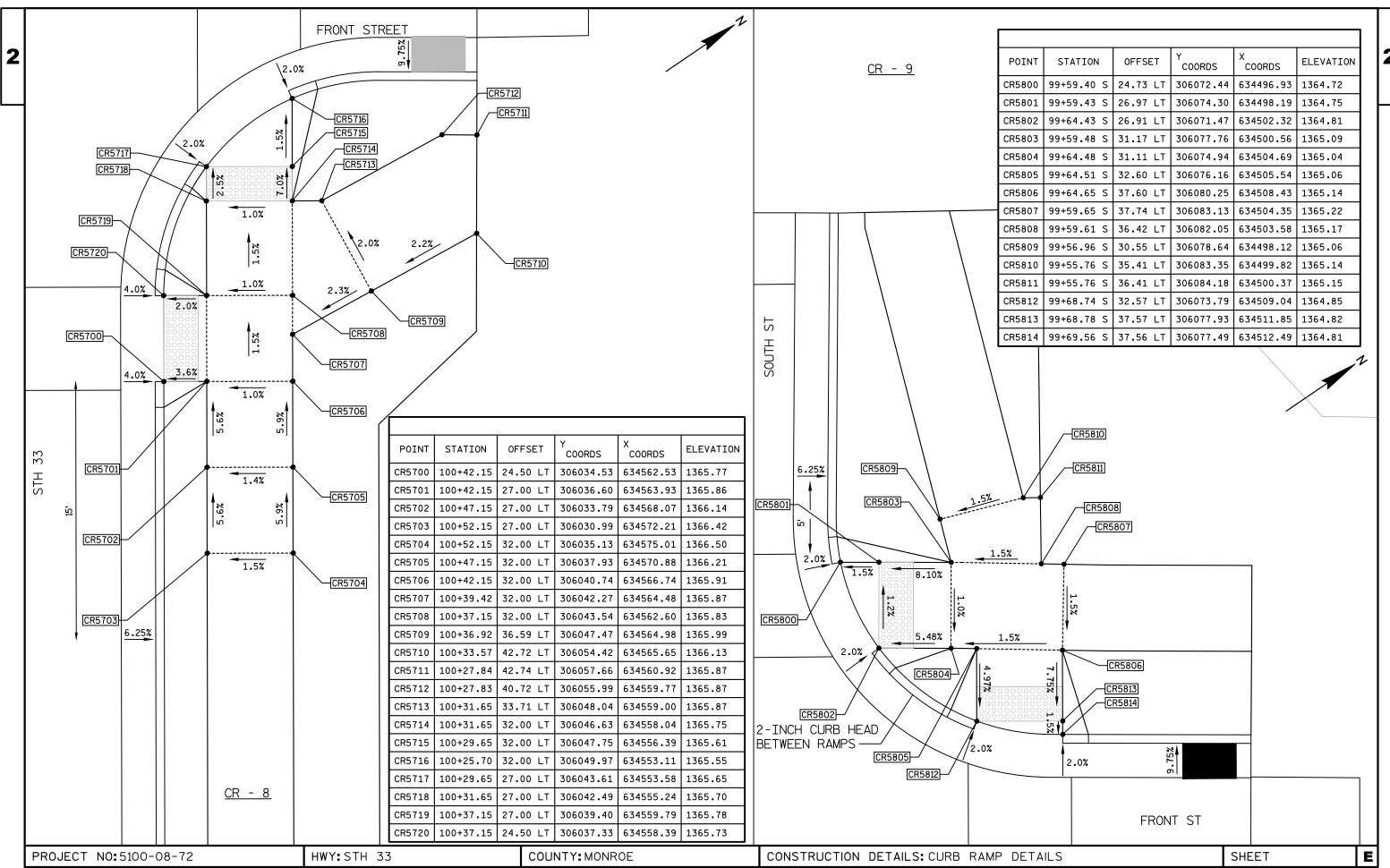
FILE NAME : S:\MAD\1000--1099\1089\837\ACAD\CIVIL 3D\SHEETSPLAN\021101_ID.DWG LAYOUT NAME - 021101_ID - 021104_ID

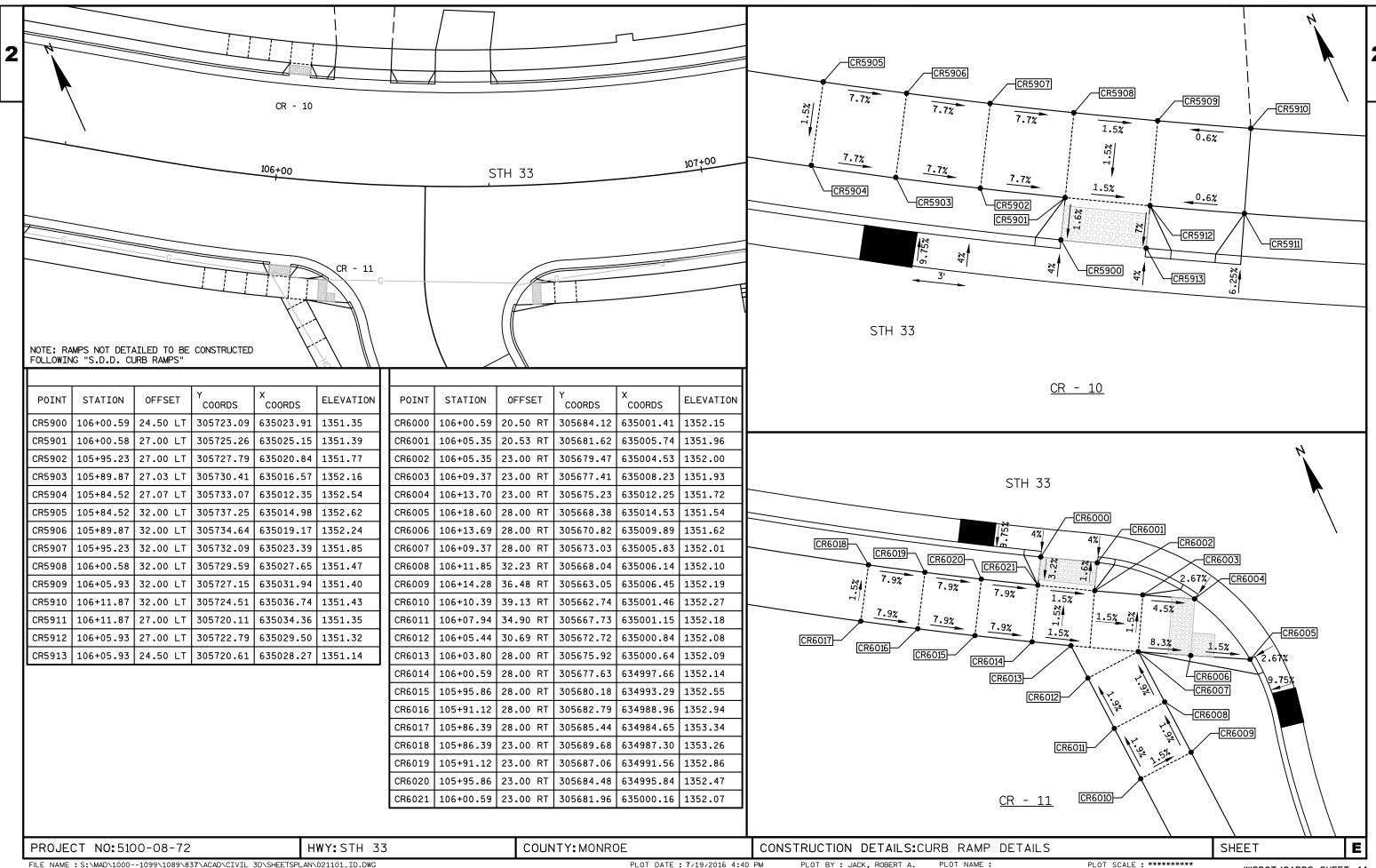
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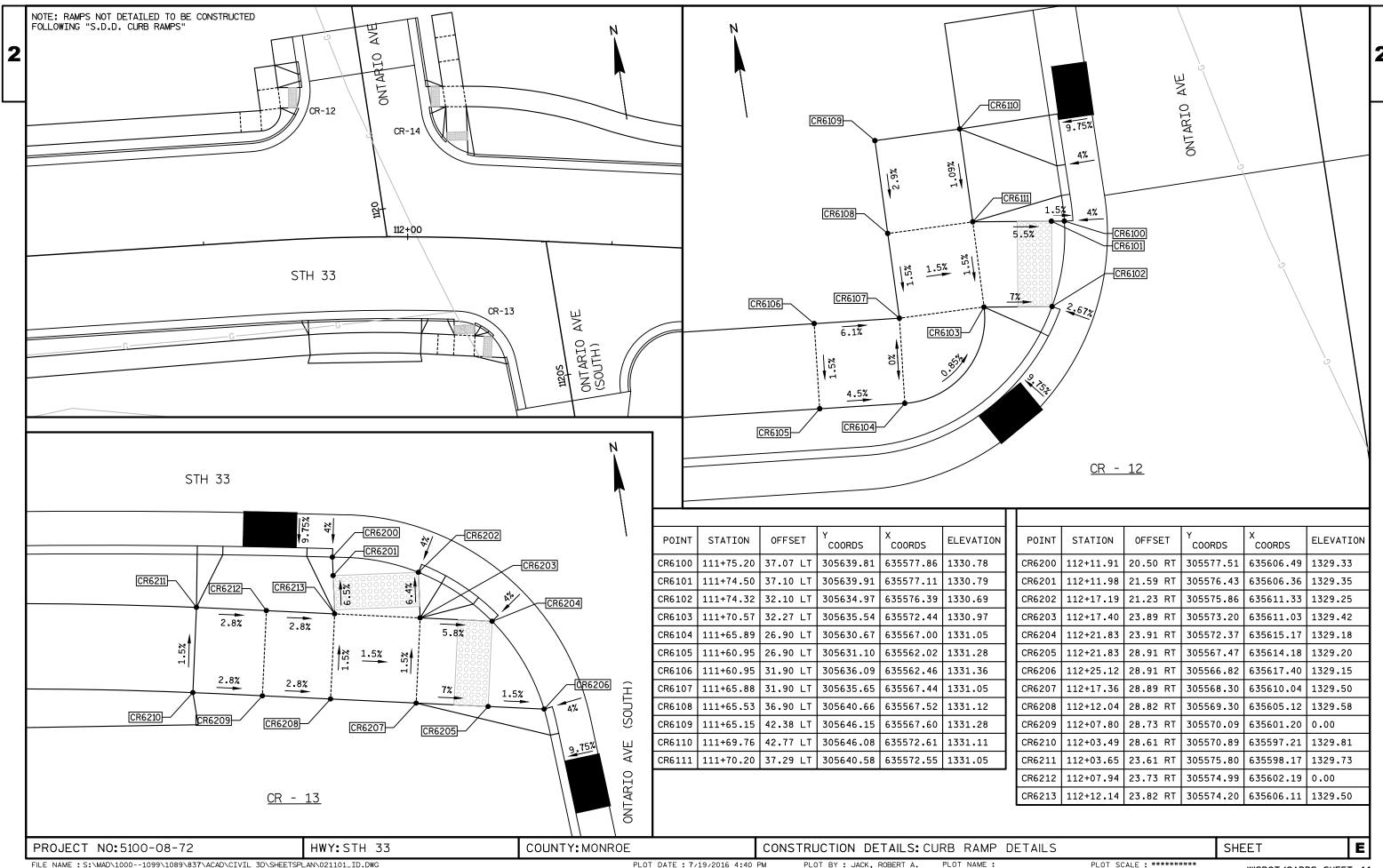
PLOT BY: JACK, ROBERT A. PLOT NAME:

PLOT SCALE : ########



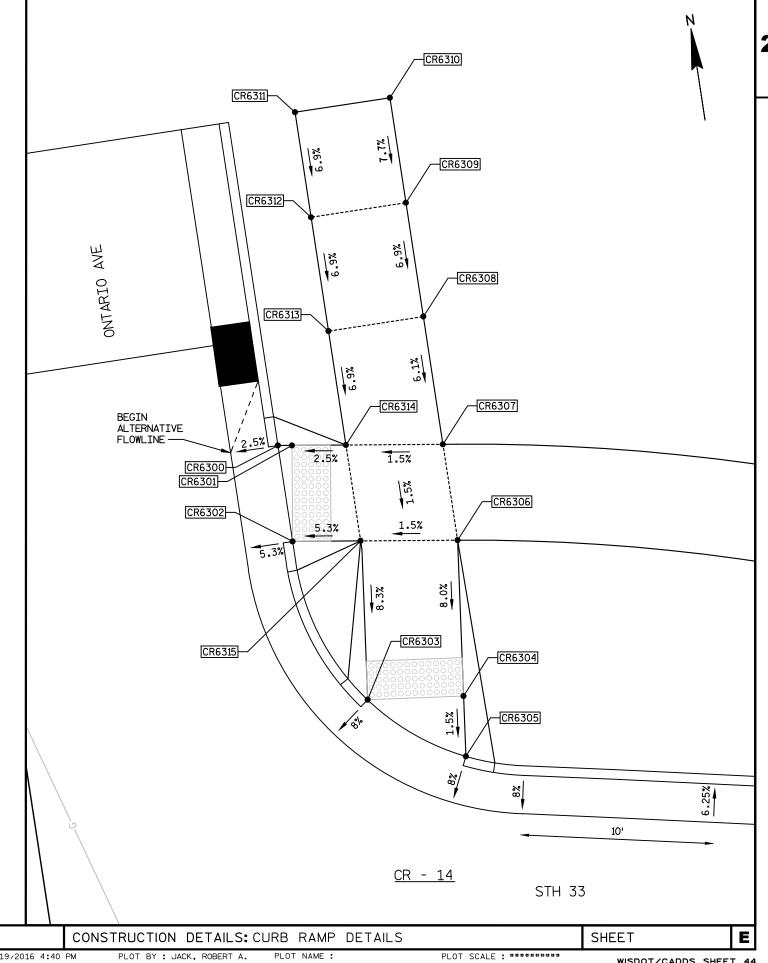






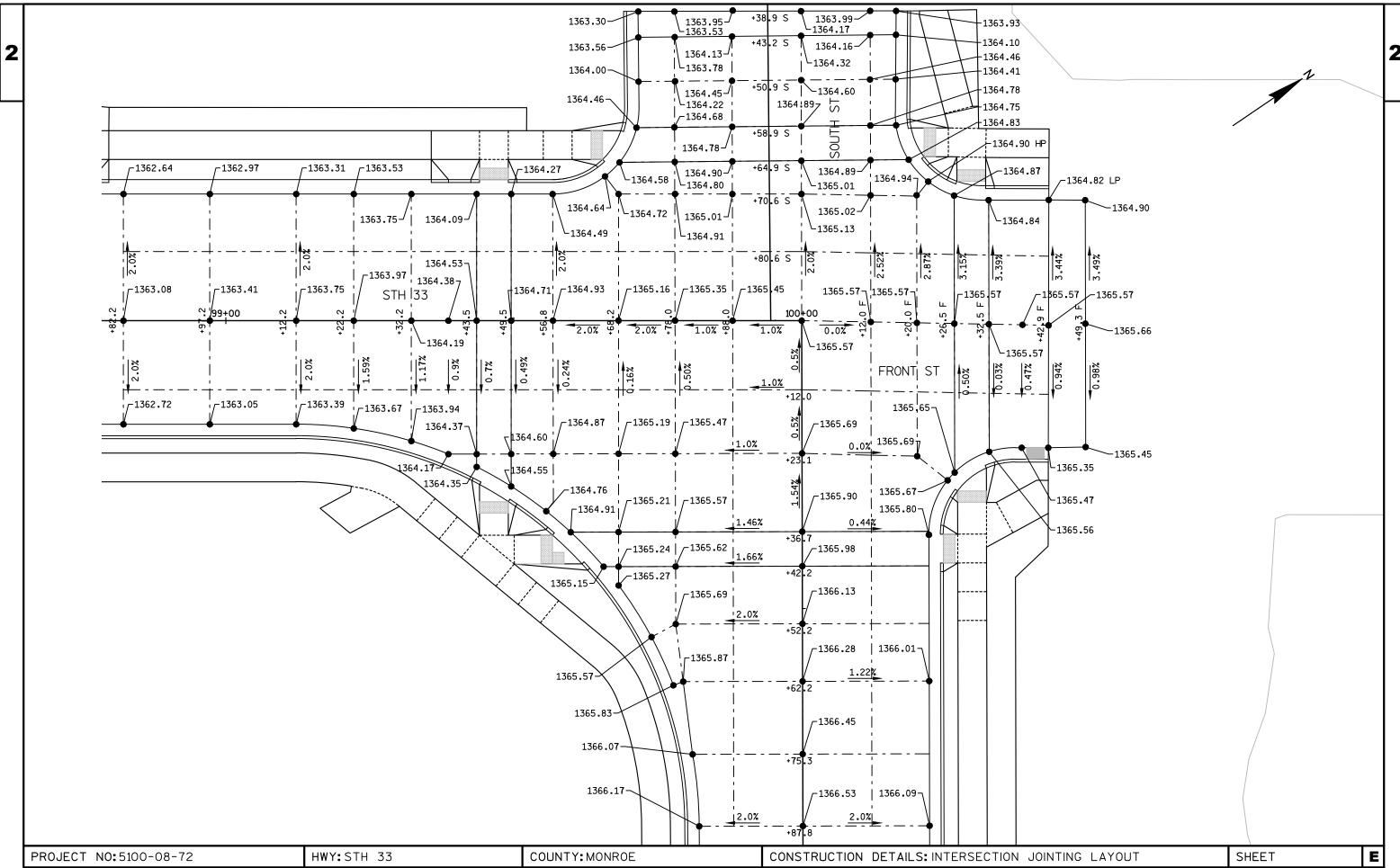
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
CR6300	112+04.47	36.84 LT	305635.29	635608.91	1330.53
CR6301	112+05.14	36.85 LT	305635.18	635609.63	1330.55
CR6302	112+05.25	31.86 LT	305630.24	635608.91	1330.36
CR6303	112+09.10	23.69 LT	305621.50	635611.52	1329.86
CR6304	112+13.87	24.02 LT	305620.93	635616.49	1329.97
CR6305	112+14.08	20.88 LT	305617.81	635616.13	1329.72
CR6306	112+13.33	32.12 LT	305629.00	635617.41	1330.62
CR6307	112+12.47	37.09 LT	305634.05	635617.41	1330.70
CR6308	112+11.35	43.71 LT	305640.78	635617.41	1331.05
CR6309	112+10.38	49.61 LT	305646.78	635617.41	1331.39
CR6310	112+09.50	55.06 LT	305652.31	635617.41	1331.95
CR6311	112+05.05	54.20 LT	305652.31	635612.41	1331.83
CR6312	112+05.89	48.75 LT	305646.78	635612.41	1331.45
CR6313	112+06.82	42.84 LT	305640.78	635612.41	1330.97
CR6314	112+07.76	36.93 LT	305634.78	635612.41	1330.62
CR6315	112+08.58	31.95 LT	305629.73	635612.41	1330.55

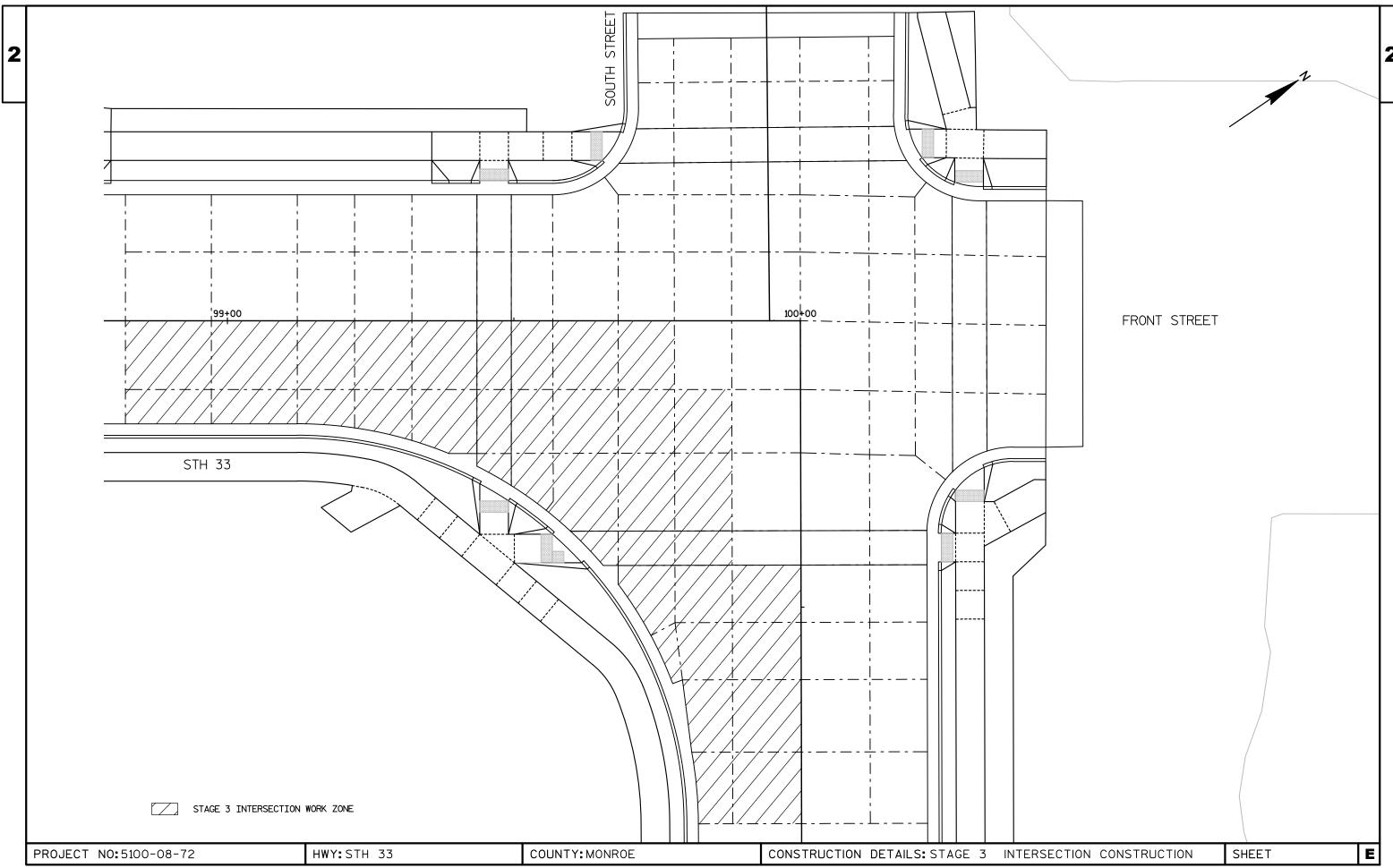
HWY:STH 33

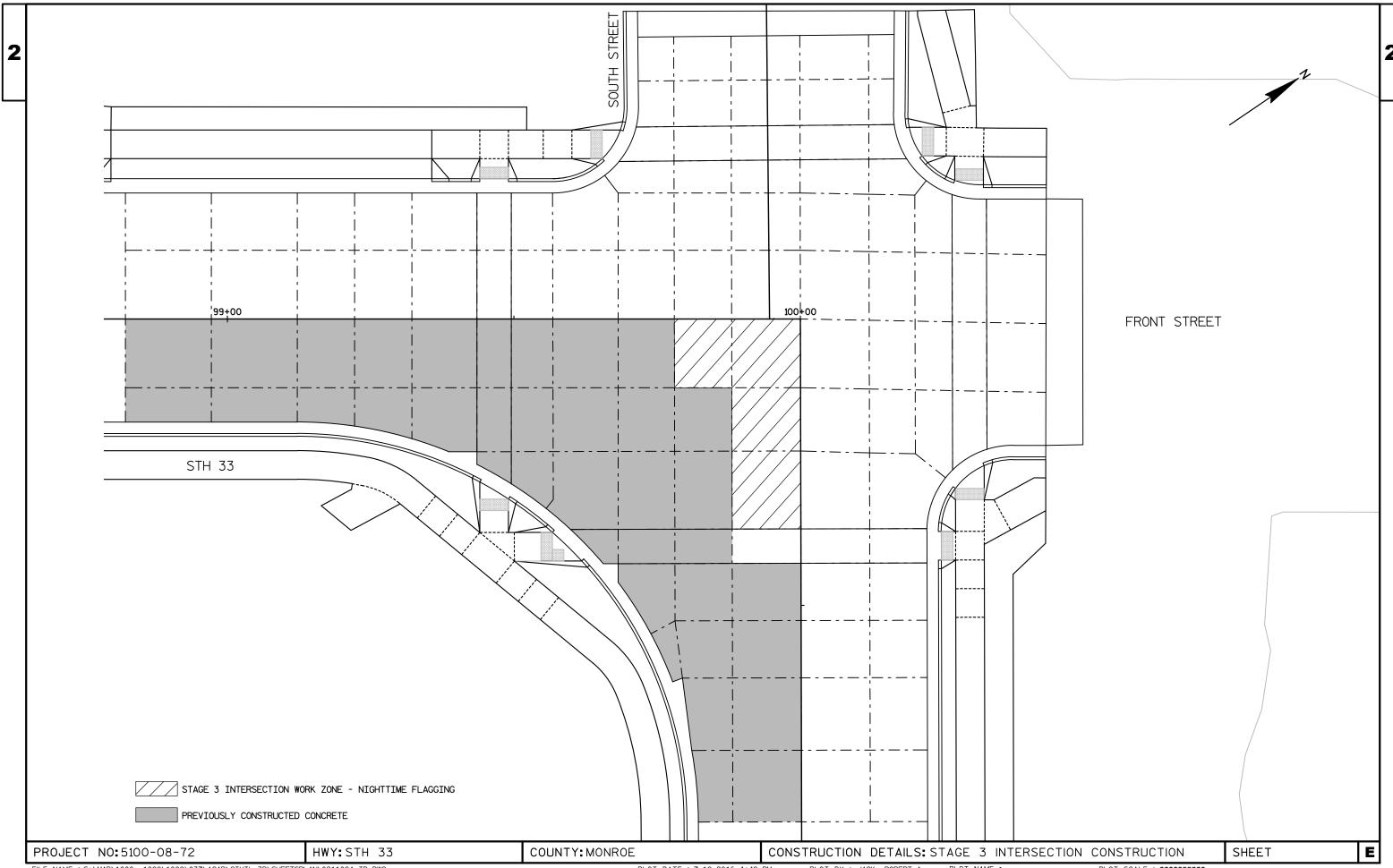


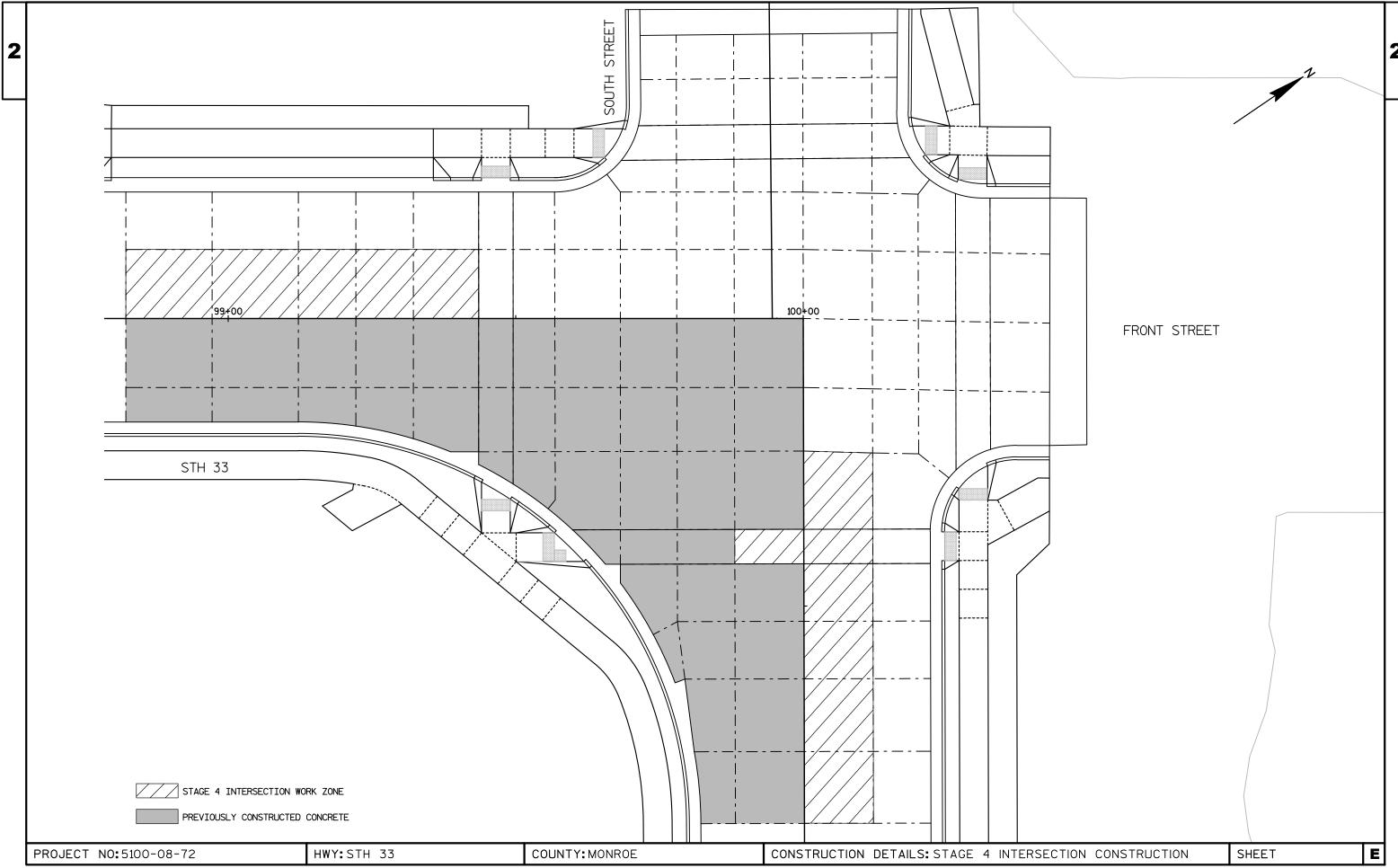
PROJECT NO:5100-08-72

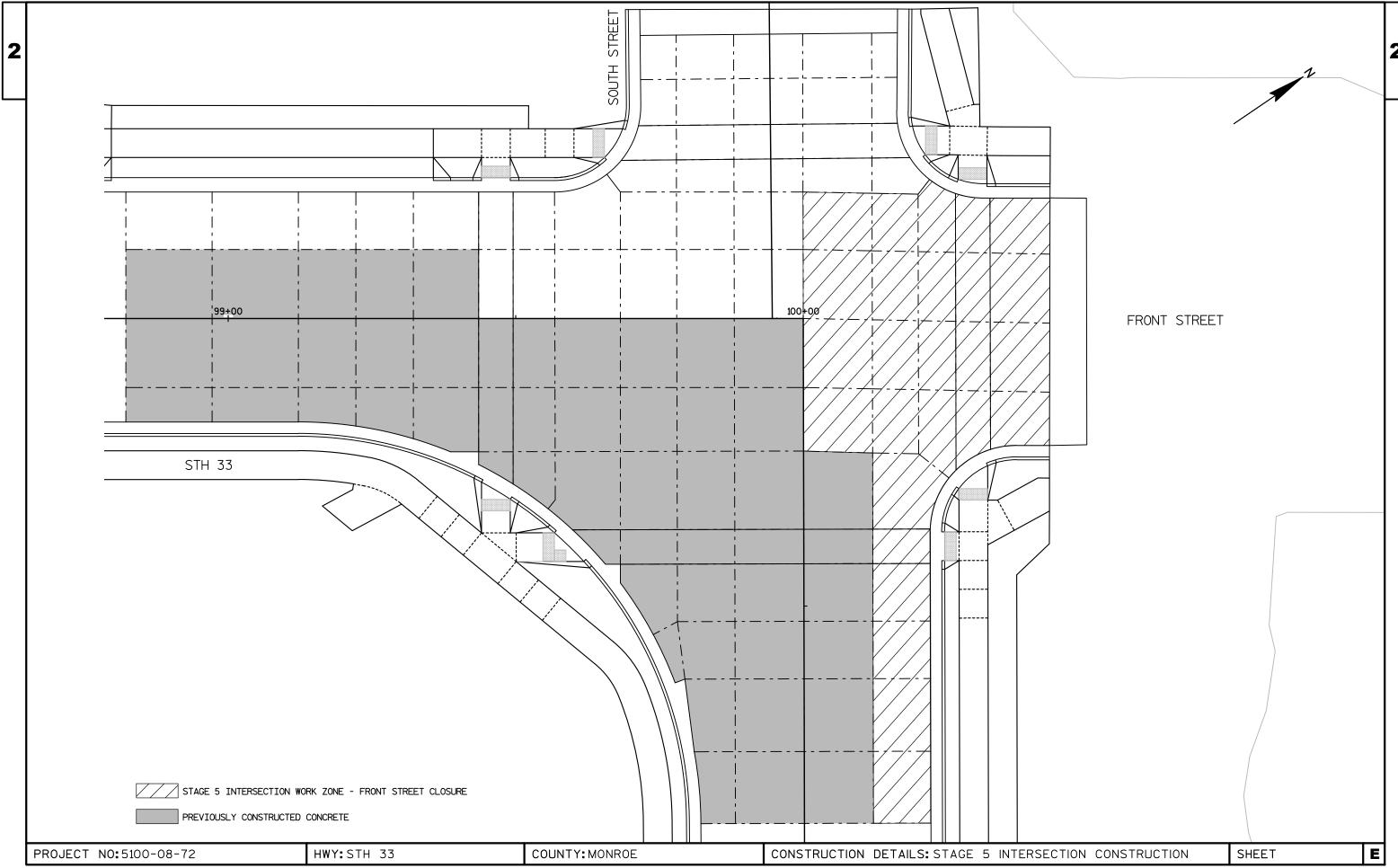
COUNTY: MONROE

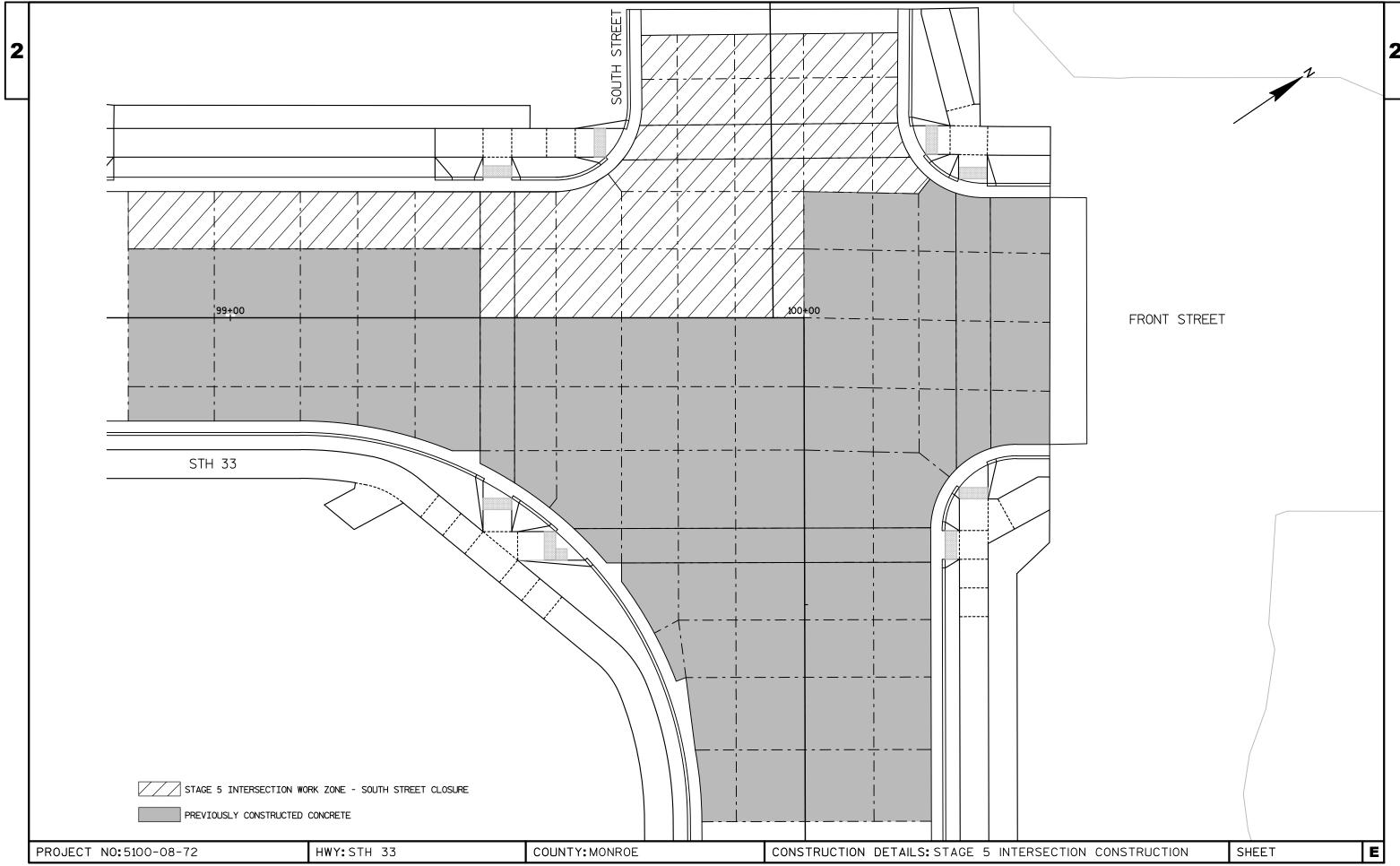


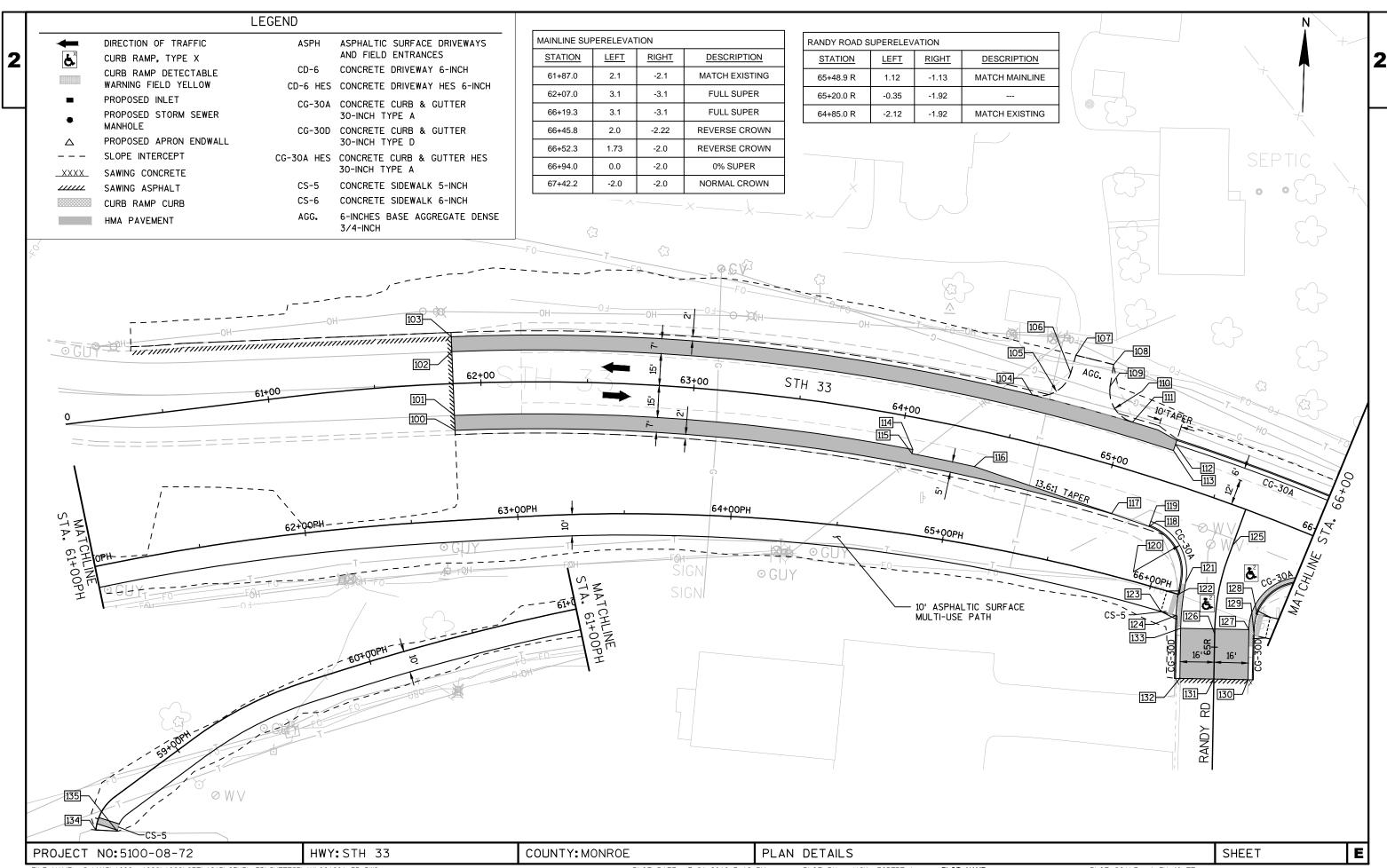






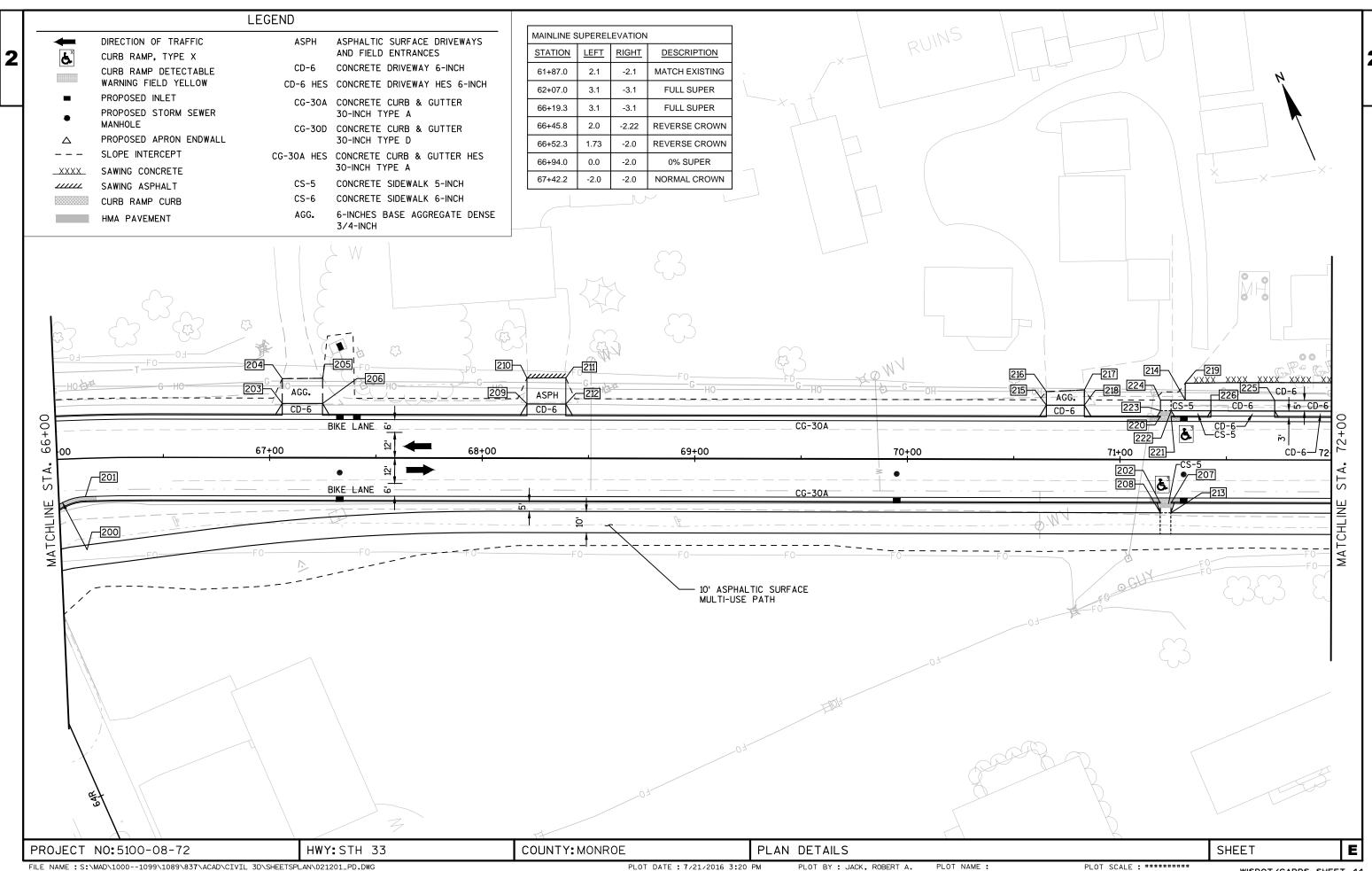






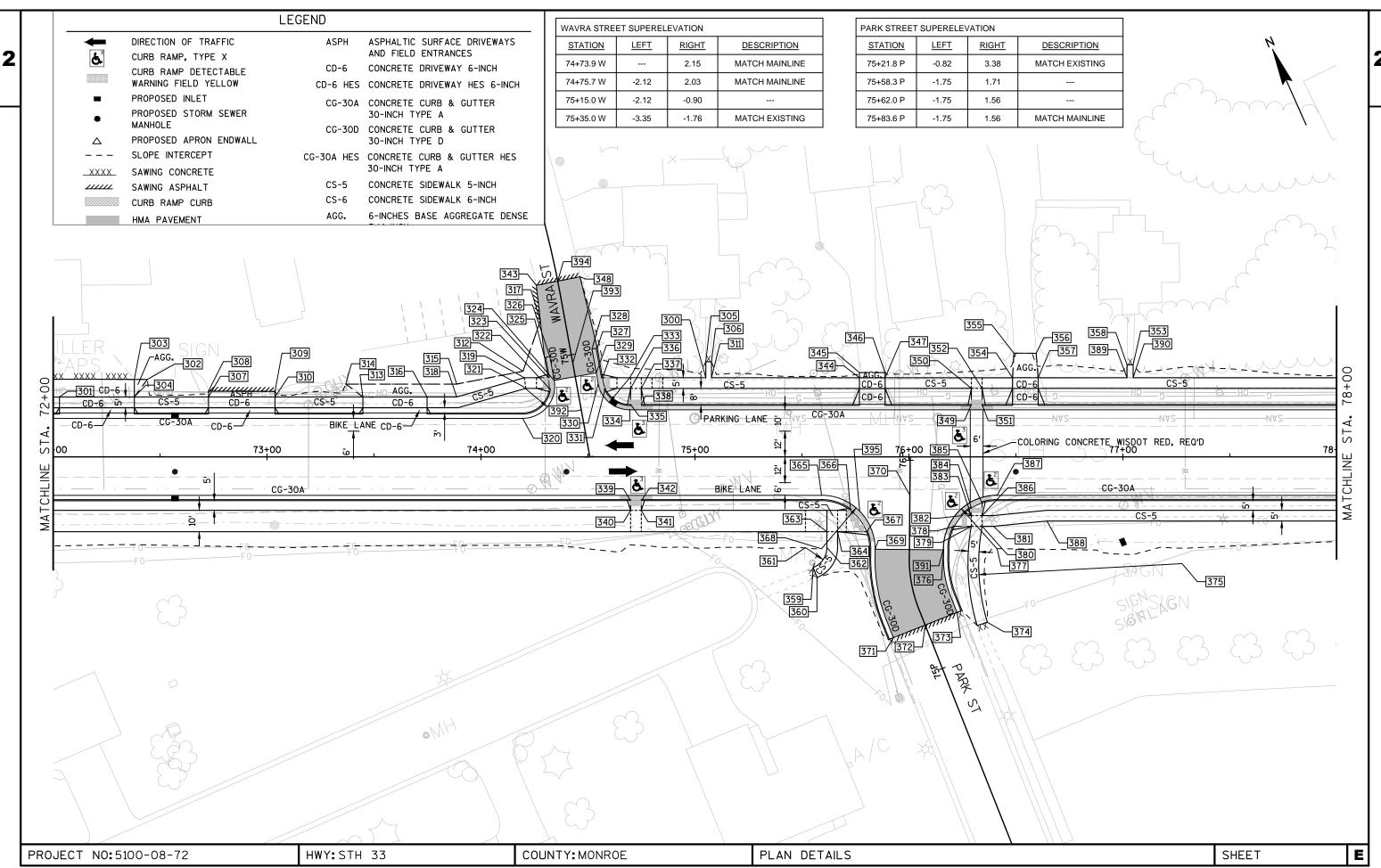
2

POINT STATION					STATION	& OFFSET T	ABLE
101	POINT	STATION	OFFSET			ELEVATION	REMARKS
102	100	61+86.96	22.00 RT	305,867.78	631,164.92	0.00	EDGE OF ASPHALT; MATCH EXISTING
103	101	61+87.00	15.00 RT	305,874.76	631,164.44	0.00	EDGE OF CONCRETE; MATCH EXISTING
104	102	61+86.96	15.00 LT	305,904.68	631,162.14	0.00	EDGE OF CONCRETE; MATCH EXISTING
105	103	61+86.96	22.00 LT	305,911.66	631,161.62	0.00	EDGE OF ASPHALT; MATCH EXISTING
106	104	64+55.73	22.00 LT	305,893.73	631,435.08	1365.62	BEGIN RADIUS
107	105	64+55.73	37.00 LT	305,908.41	631,438.15	0.00	R = 15'
108	106	64+70.16	36.36 LT	305,904.61	631,452.66	1365.10	END RADIUS
109 64+88.97 35.18 LT 305.898.99 631,471.39 1364.88 END RADIUS 110 65+03.32 37.00 LT 305.897.09 631.486.27 0.00 R = 15* 111 65+03.32 22.00 LT 305.892.58 631.492.48 1365.32 BEGIN RADIUS 112 65+25.23 18.00 LT 305.862.99 1364.96 EOP; BEGIN CONCRETE CURB & GUTTER 113 65+25.21 15.00 LT 305.869.93 631.502.14 1364.87 EDGE OF CONCRETE 114 64+06.57 15.00 RT 305.866.20 631.379.91 1364.41 EDGE OF CONCRETE 115 64+06.57 17.00 RT 305.864.22 631.379.60 1364.34 EDGE OF CONCRETE 116 64+36.95 17.00 RT 305.859.15 631.499.01 1364.43 EDGE OF CONCRETE 117 65+06.22 22.00 RT 305.893.29 631.474.07 1363.93 EOP 118 65+25.27 22.51 RT 305.833.86 631.491.87 1363.69 EOP; END TAPER 119 65+26.80 45.00 RT 305.833.86 631.491.87 1363.69 EOP; END TAPER 120 65+42.46 45.00 RT 305.832.26 631.509.96 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 121 65+49.30 48.22 RT 305.808.22 631.509.96 1363.78 EOP; END RADIUS 122 65+49.30 48.22 RT 305.808.22 631.509.96 1363.79 EOP; END RADIUS 123 65+44.04 68.49 RT 305.794.25 631.498.85 0.00 BOC; CURB RAMP OPENING 124 65+51.80 58.10 RT 305.792.45 631.505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305.792.45 631.505.92 0.00 BOC; CURB RAMP OPENING 126 65+37.25 60.04 RT 305.794.45 631.504.93 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 127 65+88.27 52.13 RT 305.794.45 631.540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+97.28 74.10 RT 305.763.58 631.540.97 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305.763.58 631.540.97 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305.763.58 631.540.97 0.00 BOC; CURB RAMP OPENING 131 65+81.01 79.77 RT 305.763.58 631.540.97 0.00 BOC; CURB RAMP OPENING 132 65+64.53 85.18 RT 305.763.58 631.540.97 0.00 BOC; CURB RAMP OPENING 133 65+66.22 62.93 RT 305.763.58 631.508.78 0.00 EOP; MATCH EXISTING 133 65+64.69 PH 0.00 RT 305.763.56 631.508.78 0.00 EOP; MATCH EXISTING	107	64+70.45	45.07 LT	305,913.04	631,454.88	0.00	MATCH EXISTING
110 65+03.32 37.00 LT 305,897.09 631,486.27 0.00 R - 15' 111 65+03.32 22.00 LT 305,882.58 631,482.48 1365.32 BEGIN RADIUS 112 65+25.23 18.00 LT 305,862.58 631,502.99 1364.96 EOP; BEGIN CONCRETE CURB & GUTTER 113 65+25.21 15.00 LT 305,869.93 631,502.14 1364.87 EDGE OF CONCRETE 114 64+06.57 15.00 RT 305,866.20 631,379.91 1364.41 EDGE OF CONCRETE 115 64+06.57 17.00 RT 305,864.22 631,379.60 1364.34 EDGE OF CONCRETE 116 64+36.95 17.00 RT 305,864.22 631,379.60 1364.34 EDGE OF CONCRETE 117 65+06.22 22.00 RT 305,839.29 631,474.07 1363.93 EOP 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.21 22.51 RT 305,833.86 631,491.87 1363.69 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,832.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,808.22 631,505.64 0.00 BOC; CURB RAMP OPENING 123 65+40.04 58.49 RT 305,794.25 631,498.85 0.00 BOC 124 65+51.80 58.10 RT 305,794.25 631,591.36 1363.49 PROFILE GRADE BREAK 125 65+66.01 18.00 RT 305,784.32 631,550.92 0.00 BOC; CURB RAMP OPENING 126 65+73.25 60.04 RT 305,784.32 631,540.39 1362.49 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,794.42 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+99.22 44.03 RT 305,794.42 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 129 65+91.76 53.76 RT 305,784.45 631,540.39 1362.61 EOP; END RAMP OPENING 130 65+97.28 74.10 RT 305,763.36 631,540.77 0.00 BOC; CURB RAMP OPENING 131 65+81.01 79.77 RT 305,763.36 631,508.40 1362.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 133 65+64.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	108	64+87.85	45.48 LT	305,909.27	631,472.71	0.00	MATCH EXISTING
111 65+03.32 22.00 LT 305,882.58 631,482.48 1365.32 BEGIN RADIUS 112 65+25.23 18.00 LT 305,872.81 631,502.99 1364.96 EOP; BEGIN CONCRETE CURB & GUTTER 113 65+25.21 15.00 LT 305,869.93 631,502.14 1364.87 EDGE OF CONCRETE 114 64+06.57 15.00 RT 305,866.20 631,379.91 1364.41 EDGE OF CONCRETE 115 64+06.57 17.00 RT 305,864.22 631,379.91 1364.41 EDGE OF CONCRETE 116 64+36.95 17.00 RT 305,864.22 631,379.00 1364.34 EDGE OF CONCRETE 117 65+06.22 22.00 RT 305,8839.29 631,474.07 1363.93 EDGE OF CONCRETE; BEGIN TAPER 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.09 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,802.25 631,509.96 1363.19 EOP; END RADIUS 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOC; CURB RAMP OPENING 124 65+51.80 58.10 RT 305,794.25 631,591.36 1363.40 PROFILE GRADE BREAK 125 65+66.08 18.00 RT 305,784.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,540.39 1362.61 EOP; END RADIUS BEGIN ASPHALTIC SURFACE 127 65+88.27 52.13 RT 305,784.32 631,540.39 1362.61 EOP; END RADIUS BEGIN ASPHALTIC SURFACE 128 65+99.22 44.03 RT 305,784.45 631,540.39 1362.61 EOP; END RADIUS BEGIN ASPHALTIC SURFACE 129 65+91.76 53.76 RT 305,784.45 631,540.39 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.36 631,540.77 0.00 BOC; CURB RAMP OPENING 131 65+81.01 79.77 RT 305,763.36 631,508.40 1362.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 133 65+66.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE	109	64+88.97	35.18 LT	305,898.99	631,471.39	1364.88	END RADIUS
112 65+25.23 18.00 LT 305,872.81 631,502.99 1364.96 EOP; BEGIN CONCRETE CURB & GUTTER 113 65+25.21 15.00 LT 305,869.93 631.502.14 1364.87 EDGE OF CONCRETE 114 64+06.57 15.00 RT 305,866.20 631,379.91 1364.41 EDGE OF CONCRETE 115 64+06.57 17.00 RT 305,864.22 631,379.60 1364.34 EDGE OF CONCRETE 116 64+36.95 17.00 RT 305,859.15 631,409.01 1364.43 EDGE OF CONCRETE; BEGIN TAPER 117 65+06.22 22.00 RT 305,839.29 631,474.07 1363.93 EOP 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,802.58 631,509.96 1363.19 EOP; END RADIUS 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+44.04 58.49 RT 305,794.25 631,509.96 1363.19 EOP; END RADIUS 123 65+44.04 58.49 RT 305,794.25 631,509.96 1363.00 BOC; CURB RAMP OPENING 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+33.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 128 65+99.22 44.03 RT 305,794.42 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+91.76 53.76 RT 305,784.35 631,540.77 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.32 631,524.77 0.00 BOC; CURB RAMP OPENING 131 65+61.03 85.18 RT 305,763.32 631,508.40 1362.77 EOP; MATCH EXISTING 133 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 134 65+64.69 PH 0.00 RT 305,866.64 630,776.48 0.00 MATCH EXISTING	110	65+03.32	37.00 LT	305,897.09	631,486.27	0.00	R = 15'
113 65+25.21 15.00 LT 305,869.93 631,502.14 1364.87 EDGE OF CONCRETE 114 64+06.57 15.00 RT 305,866.20 631,379.91 1364.41 EDGE OF CONCRETE 115 64+06.57 17.00 RT 305,864.22 631,379.60 1364.34 EDGE OF CONCRETE 116 64+36.95 17.00 RT 305,859.15 631,409.01 1364.43 EDGE OF CONCRETE; BEGIN TAPER 117 65+06.22 22.00 RT 305,839.29 631,474.07 1363.93 EOP 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,832.24 631,495.30 0.00 R - 25' 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOC 124 65+51.80 58.10 RT 305,794.25 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,784.32 631,505.92 0.00 BOC; CURB RAMP OPENING 126 65+37.25 60.04 RT 305,784.32 631,531.36 1363.40 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,784.42 631,543.98 0.00 BOC; CURB RAMP OPENING 128 65+89.22 44.03 RT 305,784.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,543.99 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 BOC; CURB RAMP OPENING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,786.69 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	111	65+03.32	22.00 LT	305,882.58	631,482.48	1365.32	BEGIN RADIUS
114 64+06.57 15.00 RT 305,866.20 631,379.91 1364.41 EDGE OF CONCRETE 115 64+06.57 17.00 RT 305,864.22 631,379.60 1364.34 EDGE OF CONCRETE 116 64+36.95 17.00 RT 305,859.15 631,409.01 1364.43 EDGE OF CONCRETE; BEGIN TAPER 117 65+06.22 22.00 RT 305,839.29 631,474.07 1363.93 EOP 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,812.34 631,485.30 0.00 R - 25' 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,782.45 631,505.92 0.00 BOC; CURB RAMP OPENING 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+91.76 53.76 RT 305,784.45 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,783.32 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+61.01 79.77 RT 305,763.32 631,508.78 0.00 EOP; MATCH EXISTING 133 65+62.22 62.93 RT 305,786.69 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	112	65+25.23	18.00 LT	305,872.81	631,502.99	1364.96	EOP; BEGIN CONCRETE CURB & GUTTER
115 64+06.57 17.00 RT 305,864.22 631,379.60 1364.34 EDGE OF CONCRETE 116 64+36.95 17.00 RT 305,859.15 631,409.01 1364.43 EDGE OF CONCRETE; BEGIN TAPER 117 65+06.22 22.00 RT 305,839.29 631,474.07 1363.93 EOP 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,812.34 631,485.30 0.00 R = 25' 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,509.96 1363.19 EOP; END RADIUS 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOC; CURB RAMP OPENING 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,763.58 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 EOP; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	113	65+25.21	15.00 LT	305,869.93	631,502.14	1364.87	EDGE OF CONCRETE
116 64+36.95 17.00 RT 305,859.15 631,409.01 1364.43 EDGE OF CONCRETE; BEGIN TAPER 117 65+06.22 22.00 RT 305,839.29 631,474.07 1363.93 EOP 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,836.25 631,492.60 1363.78 EOP; END RADIUS 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,509.96 1363.19 EOP; END RADIUS 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOC; CURB RAMP OPENING 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+91.76 53.76 RT 305,784.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,763.58 631,540.77 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,763.06 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	114	64+06.57	15.00 RT	305,866.20	631,379.91	1364.41	EDGE OF CONCRETE
117 65+06.22 22.00 RT 305,839.29 631,474.07 1363.93 EOP 118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,812.34 631,485.30 0.00 R - 25' 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOC 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,763.58 631,540.77 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 BOC; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	115	64+06.57	17.00 RT	305,864.22	631,379.60	1364.34	EDGE OF CONCRETE
118 65+25.27 22.51 RT 305,833.86 631,491.87 1363.69 EOP; END TAPER 119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,812.34 631,485.30 0.00 R = 25' 121 65+50.84 41.85 RT 305,802.25 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOS 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+93.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,763.32 631,540.77 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.32 631,524.77 0.00 BCC; CURB RAMP OPENING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	116	64+36.95	17.00 RT	305,859.15	631,409.01	1364.43	EDGE OF CONCRETE; BEGIN TAPER
119 65+25.31 20.01 RT 305,836.25 631,492.60 1363.78 EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS 120 65+24.86 45.00 RT 305,812.34 631,485.30 0.00 R = 25 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOS 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,763.58 631,540.77 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 131 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,766.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	117	65+06.22	22.00 RT	305,839.29	631,474.07	1363.93	EOP
120 65+24.86 45.00 RT 305,812.34 631,485.30 0.00 R = 25' 121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOS 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,786.59 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE	118	65+25.27	22.51 RT	305,833.86	631,491.87	1363.69	EOP; END TAPER
121 65+50.84 41.85 RT 305,808.22 631,509.96 1363.19 EOP; END RADIUS 122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOS 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	119	65+25.31	20.01 RT	305,836.25	631,492.60	1363.78	EOP; BEGIN CONCRETE CURB & GUTTER; BEGIN RADIUS
122 65+49.30 48.22 RT 305,802.58 631,506.64 0.00 BOC; CURB RAMP OPENING 123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOS 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	120	65+24.86	45.00 RT	305,812.34	631,485.30	0.00	R = 25'
123 65+44.04 58.49 RT 305,794.25 631,498.85 0.00 BOS 124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	121	65+50.84	41.85 RT	305,808.22	631,509.96	1363.19	EOP; END RADIUS
124 65+51.80 58.10 RT 305,792.45 631,505.92 0.00 BOC; CURB RAMP OPENING 125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	122	65+49.30	48.22 RT	305,802.58	631,506.64	0.00	BOC; CURB RAMP OPENING
125 65+66.08 18.00 RT 305,826.35 631,531.36 1363.40 PROFILE GRADE BREAK 126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	123	65+44.04	58.49 RT	305,794.25	631,498.85	0.00	BOS
126 65+73.25 60.04 RT 305,784.32 631,524.44 1362.89 PROFILE GRADE BREAK 127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	124	65+51.80	58.10 RT	305,792.45	631,505.92	0.00	BOC; CURB RAMP OPENING
127 65+88.27 52.13 RT 305,787.10 631,540.39 1362.61 EOP; END RADIUS; BEGIN ASPHALTIC SURFACE 128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	125	65+66.08	18.00 RT	305,826.35	631,531.36	1363.40	PROFILE GRADE BREAK
128 65+89.22 44.03 RT 305,794.42 631,543.98 0.00 BOC; CURB RAMP OPENING 129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	126	65+73.25	60.04 RT	305,784.32	631,524.44	1362.89	PROFILE GRADE BREAK
129 65+91.76 53.76 RT 305,784.45 631,542.94 0.00 BOC; CURB RAMP OPENING 130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	127	65+88.27	52.13 RT	305,787.10	631,540.39	1362.61	EOP; END RADIUS; BEGIN ASPHALTIC SURFACE
130 65+97.28 74.10 RT 305,763.58 631,540.77 0.00 EOP; MATCH EXISTING 131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	128	65+89.22	44.03 RT	305,794.42	631,543.98	0.00	BOC; CURB RAMP OPENING
131 65+81.01 79.77 RT 305,763.32 631,524.77 0.00 BEGIN PROFILE; MATCH EXISTING 132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	129	65+91.76	53.76 RT	305,784.45	631,542.94	0.00	BOC; CURB RAMP OPENING
132 65+64.53 85.18 RT 305,763.06 631,508.78 0.00 EOP; MATCH EXISTING 133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	130	65+97.28	74.10 RT	305,763.58	631,540.77	0.00	EOP; MATCH EXISTING
133 65+56.22 62.93 RT 305,786.59 631,508.40 1362.77 EOP; BEGIN ASPHALTIC SURFACE 134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	131	65+81.01	79.77 RT	305,763.32	631,524.77	0.00	BEGIN PROFILE; MATCH EXISTING
134 58+44.69 PH 0.00 RT 305,686.64 630,776.48 0.00 MATCH EXISTING	132	65+64.53	85.18 RT	305,763.06	631,508.78	0.00	EOP; MATCH EXISTING
	133	65+56.22	62.93 RT	305,786.59	631,508.40	1362.77	EOP; BEGIN ASPHALTIC SURFACE
135 58+45.54 PH 10.00 RT 305,686.48 630,786.50 0.00 MATCH EXISTING	134	58+44.69 PH	0.00 RT	305,686.64	630,776.48	0.00	MATCH EXISTING
	135	58+45.54 PH	10.00 RT	305,686.48	630,786.50	0.00	MATCH EXISTING



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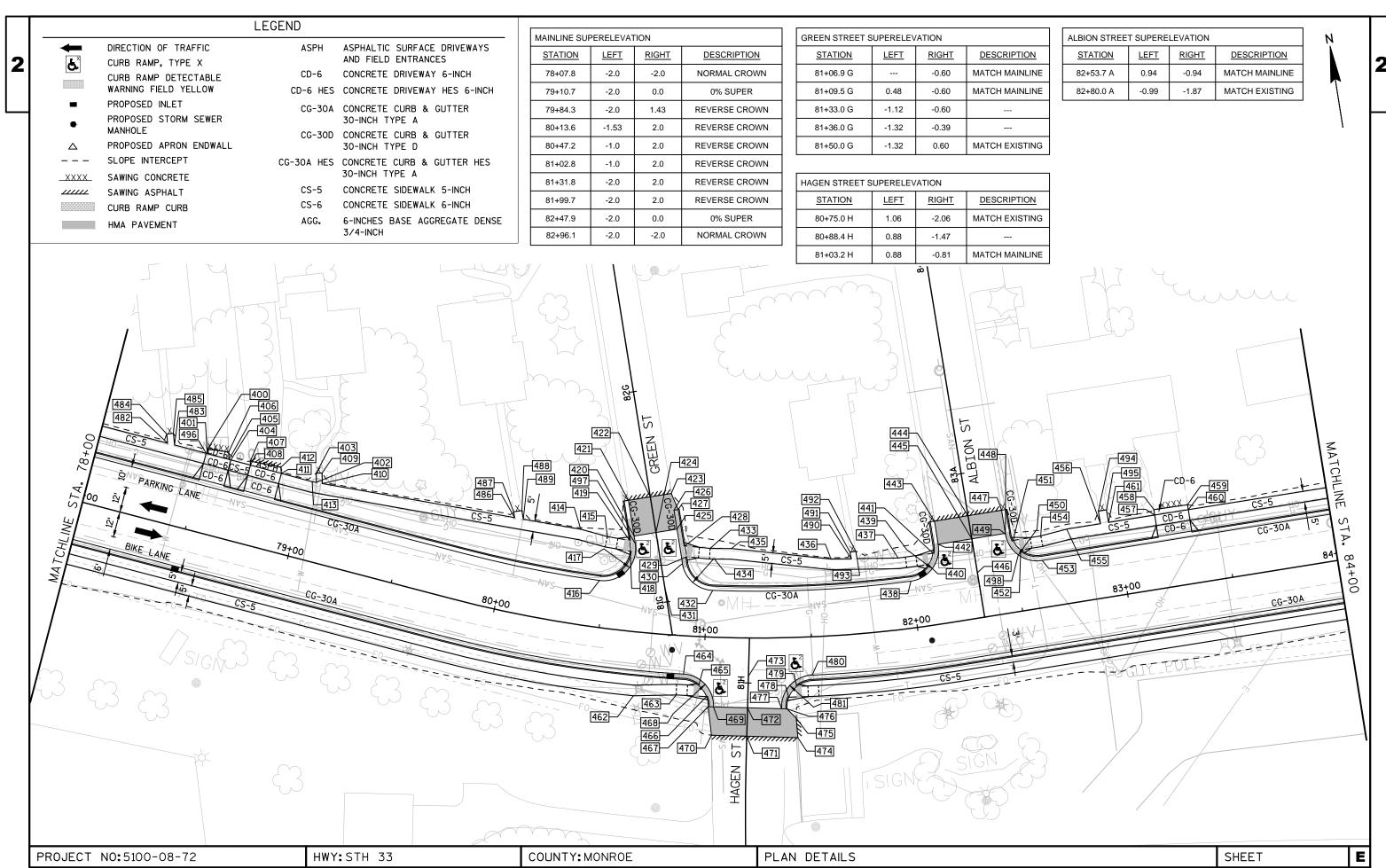
	STATION & OFFSET TABLE											
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS						
200	66+12.77	43.00 RT	305,787.50	631,565.39	0.00	R = 25'						
201	66+12.77	18.00 RT	305,810.80	631,574.45	1362.88	EOP; BEGIN RADIUS						
202	71+18.99	20.50 RT	305,610.43	632,038.67	0.00	BOC; CURB RAMP OPENING						
203	67+05.85	25.50 LT	305,814.96	631,676.79	1362.64	LIMITS OF CONCRETE DRIVEWAY 6-INCH						
204	67+05.85	37.14 LT	305,825.66	631,681.36	0.00	MATCH EXISTING						
205	67+24.83	37.39 LT	305,818.44	631,698.90	0.00	MATCH EXISTING						
206	67+24.83	25.50 LT	305,807.51	631,694.24	1363.51	LIMITS OF CONCRETE DRIVEWAY 6-INCH						
207	71+23.99	20.50 RT	305,608.47	632,043.27	0.00	BOC; CURB RAMP OPENING						
208	71+18.99	25.00 RT	305,606.29	632,036.91	0.00	FRONT OF SIDEWALK						
209	68+20.99	25.50 LT	305,769.75	631,782.67	1363.66	LIMITS OF CONCRETE DRIVEWAY 6-INCH						
210	68+20.99	37.67 LT	305,780.95	631,787.46	0.00	MATCH EXISTING						
211	68+39.23	37.85 LT	305,773.95	631,804.30	0.00	MATCH EXISTING						
212	68+39.23	25.50 LT	305,762.59	631,799.45	1363.79	LIMITS OF CONCRETE DRIVEWAY 6-INCH						
213	71+23.99	25.00 RT	305,604.33	632,041.50	0.00	FRONT OF SIDEWALK						
214	71+30.62	28.00 LT	305,650.47	632,068.42	0.00	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH BEHIND SIDEWALK						
215	70+65.30	25.50 LT	305,673.82	632,007.36	0.00	LIMITS OF CONCRETE DRIVEWAY 6-INCH						
216	70+65.30	31.86 LT	305,679.67	632,009.86	0.00	MATCH EXISTING						
217	70+83.18	32.72 LT	305,673.43	632,026.64	0.00	MATCH EXISTING						
218	70+83.18	25.50 LT	305,666.79	632,023.81	0.00	LIMITS OF CONCRETE DRIVEWAY 6-INCH						
219	71+30.61	36.02 LT	305,657.85	632,071.56	0.00	SAWCUT; MATCH EXISTING						
220	71+18.99	20.50 LT	305,648.14	632,054.77	0.00	BOC; CURB RAMP OPENING						
221	71+23.99	20.50 LT	305,646.17	632,059.37	0.00	BOC; CURB RAMP OPENING						
222	71+23.99	23.00 LT	305,648.47	632,060.35	0.00	FRONT OF SIDEWALK						
223	71+18.99	23.00 LT	305,650.44	632,055.75	0.00	FRONT OF SIDEWALK						
224	71+18.99	28.00 LT	305,655.03	632,057.72	0.00	BOS						
225	71+72.72	28.00 LT	305,633.93	632,107.13	1364.58	BOS; END CONCRETE DRIVEWAY 6-INCH						
226	71+42.70	28.00 LT	305,645.72	632,079.52	1364.48	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH						



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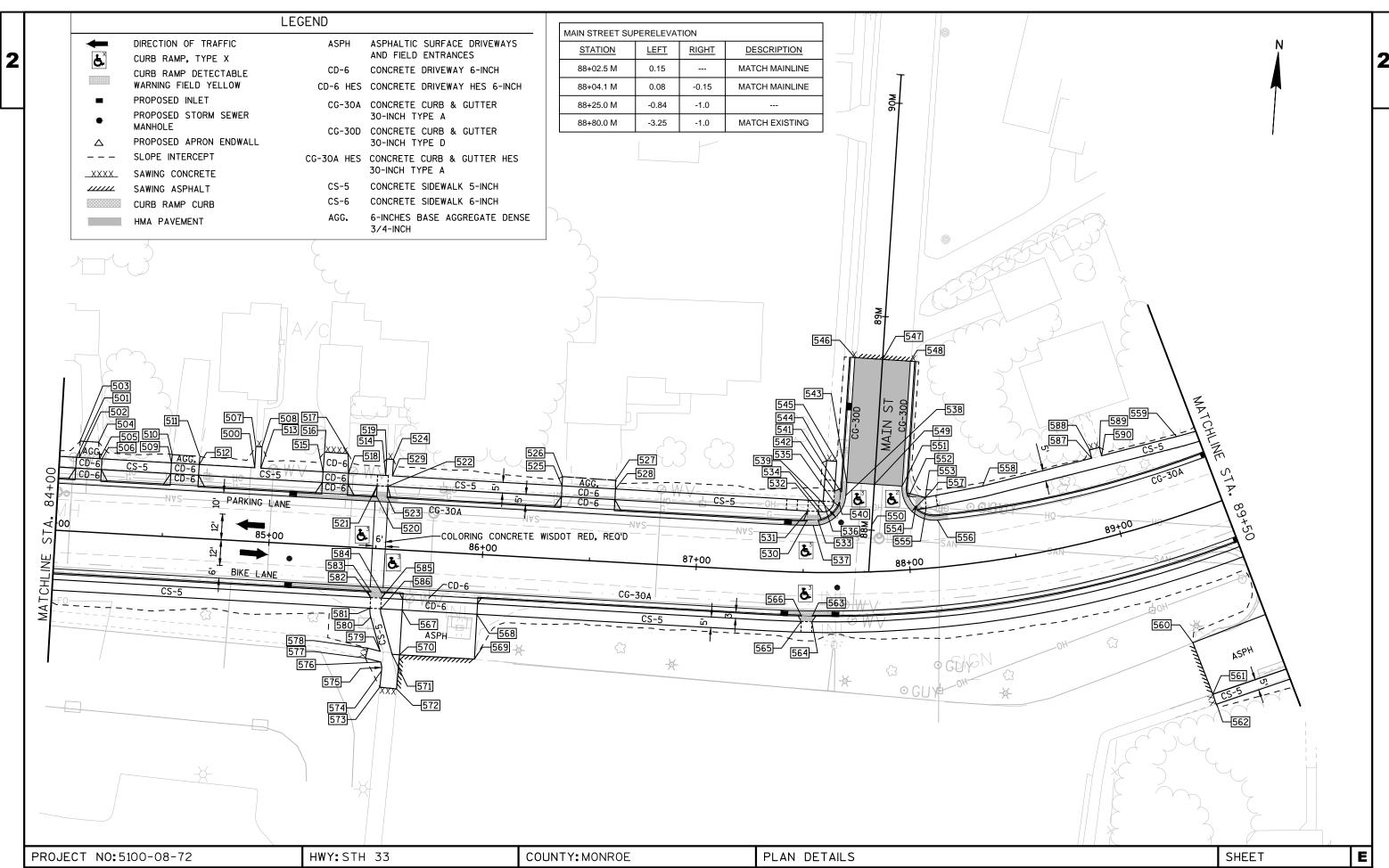
				STATION &	OFFSET TAB	LE
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
300	75+05.02	37.00 LT	305,511.73	632,416.28	1370.08	BOS
301	72+02.96	28.00 LT	305,622.06	632,134.94	1365.07	BOS
302	72+44.04	36.15 LT	305,613.42	632,175.93	0.00	MATCH EXISTING
303	72+37.97	36.18 LT	305,615.83	632,170.36	0.00	SAWCUT; MATCH EXISTING
304	72+37.95	28.00 LT	305,608.32	632,167.12	1365.08	BOS
305	75+05.02	44.47 LT	305,518.60	632,419.21	0.00	SAWCUT; MATCH EXISTING
306	75+07.71	44.47 LT	305,517.54	632,421.68	0.00	SAWCUT; MATCH EXISTING
307	72+72.68	28.00 LT	305,594.68	632,199.07	1365.49	BOS
308	72+72.68	30.70 LT	305,597.16	632,200.13	0.00	SAWCUT; MATCH EXISTING
309	73+03.74	30.55 LT	305,584.83	632,228.64	0.00	SAWCUT; MATCH EXISTING
310	73+03.74	28.00 LT	305,582.49	632,227.63	1365.90	BOS
311	75+07.71	37.00 LT	305,510.67	632,418.75	1370.13	BOS
312	74+19.65	40.02 LT	305,548.02	632,338.95	0.00	MATCH EXISTING
313	73+44.77	28.00 LT	305,566.38	632,265.36	1366.56	BOS
314	73+36.82	34.00 LT	305,575.02	632,260.41	0.00	MATCH EXISTING
315	73+88.62	34.00 LT	305,554.67	632,308.05	0.00	MATCH EXISTING
316	73+74.77	28.00 LT	305,554.60	632,292.95	1367.31	BOS
317	74+27.72	63.71 LT	305,566.65	632,355.68	0.00	EOP; SAWCUT; MATCH EXISTING
318	73+88.62	28.00 LT	305,549.16	632,305.69	1367.59	BOS
319	74+20.23	37.00 LT	305,545.02	632,338.30	0.00	BOS
320	74+19.44	18.00 LT	305,527.86	632,330.12	1367.60	EOP; BEGIN RADIUS
321	74+19.44	33.00 LT	305,541.65	632,336.01	0.00	R = 15'
322	74+31.90	32.00 LT	305,535.84	632,347.07	0.00	BOC; CURB RAMP OPENING
323	74+31.42	37.00 LT	305,540.63	632,348.59	0.00	BOC; CURB RAMP OPENING
324	74+34.20	35.69 LT	305,538.33	632,350.63	1367.37	EOP; END RADIUS; BEGIN ASPHALTIC SURFACE
325	74+29.72	46.37 LT	305,549.91	632,350.70	1368.74	EOP
326	74+32.18	46.82 LT	305,549.36	632,353.14	1368.36	EOP; END CONCRETE CURB & GUTTER
327	74+54.31	50.85 LT	305,544.37	632,375.08	1368.99	EOP
328	74+51.85	50.40 LT	305,544.93	632,372.64	1368.61	EOP; END CONCRETE CURB & GUTTER
329	74+53.88	39.27 LT	305,533.90	632,370.14	1368.36	EOP; BEGIN ASPHALTIC SURFACE
330	74+54.78	34.31 LT	305,528.98	632,369.02	1368.32	EOP; END RADIUS
331	74+58.08	32.00 LT	305,525.56	632,371.15	0.00	BOC; CURB RAMP OPENING
332	74+56.83	37.00 LT	305,530.65	632,371.96	0.00	BOC; CURB RAMP OPENING
333	74+69.54	37.00 LT	305,525.66	632,383.65	0.00	R = 15'
334	74+69.55	22.00 LT	305,511.86	632,377.77	1368.43	EOP; BEGIN RADIUS
335	74+69.90	24.50 LT	305,514.02	632,379.07	0.00	BOC; CURB RAMP OPENING
336	74+69.90	32.00 LT	305,520.92	632,382.02	0.00	
337	74+74.90	32.00 LT	305,518.96	632,386.61	0.00	
338	74+74.90	24.50 LT	305,512.06	632,383.67	0.00	BOC; CURB RAMP OPENING
339	74+69.90	20.50 RT	305,472.64	632,361.40	0.00	BOC; CURB RAMP OPENING
340	74+69.90	25.00 RT	305,468.50	632,359.63	0.00	BOC; CURB RAMP OPENING
341	74+74.90	25.00 RT	305,466.54	632,364.23	0.00	
342	74+74.90	20.50 RT	305,470.67	632,366.00	0.00	
343	74+25.83	80.15 LT	305,582.50	632,360.39	0.00	EOP; SAWCUT; MATCH EXISTING
344	75+76.83	37.00 LT	305,483.53	632,482.32	1371.09	BOS
345	75+76.83	39.36 LT	305,485.70	632,483.25	0.00	MATCH EXISTING
346	75+88.83	39.36 LT	305,480.98	632,494.29	0.00	MATCH EXISTING
347	75+88.83	37.00 LT	305,478.82	632,493.36	1371.31	BOS

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
348	74+46.25	84.16 LT	305,578.18	632,380.75	0.00	EOP; SAWCUT; MATCH EXISTING
349	76+29.16	24.50 LT	305,451.49	632,525.53	0.00	BOC; CURB RAMP OPENING
350	76+29.16	32.00 LT	305,458.39	632,528.48	0.00	
351	76+34.16	24.50 LT	305,449.52	632,530.13	0.00	BOC; CURB RAMP OPENING
352	76+34.16	32.00 LT	305,456.42	632,533.08	0.00	
353	77+04.79	43.04 LT	305,438.84	632,602.38	0.00	SAWCUT; MATCH EXISTING
354	76+48.39	37.00 LT	305,455.43	632,548.14	1372.16	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
355	76+49.02	48.48 LT	305,465.74	632,553.22	0.00	MATCH EXISTING
356	76+59.46	48.40 LT	305,461.57	632,562.79	0.00	MATCH EXISTING
357	76+60.39	36.99 LT	305,450.70	632,559.17	1372.27	BOS; END CONCRETE DRIVEWAY 6-INCH
358	77+01.91	43.04 LT	305,439.97	632,599.73	0.00	SAWCUT; MATCH EXISTING
359	75+55.27	51.77 RT	305,410.36	632,427.63	0.00	BEGIN RADIUS; SAWCUT; MATCH EXISTING
360	75+57.08	56.43 RT	305,405.36	632,427.47	0.00	SAWCUT; MATCH EXISTING
361	75+51.65	42.45 RT	305,420.35	632,427.97	0.00	R = 10'
362	75+61.65	42.45 RT	305,416.43	632,437.16	0.00	BOS; END RADIUS
363	75+61.65	35.00 RT	305,423.27	632,440.09	0.00	BOS
364	75+66.65	35.00 RT	305,421.31	632,444.69	0.00	BOS
365	75+59.21	18.00 RT	305,439.87	632,444.52	1370.14	EOP; BEGIN RADIUS
366	75+72.71	25.00 RT	305,428.13	632,454.18	0.00	BOC; CURB RAMP OPENING
367	75+80.24	35.00 RT	305,415.98	632,457.18	0.00	BOC; CURB RAMP OPENING
368	75+59.21	43.00 RT	305,416.88	632,434.70	0.00	R = 25'
369	75+84.21	43.22 RT	305,406.86	632,457.60	1370.24	EOP; END RADIUS; BEGIN ASPHALTIC SURFACE
370	76+00.43	18.00 RT	305,423.68	632,482.42	1370.88	PROFILE GRADE BREAK
371	75+92.72	84.54 RT	305,365.51	632,449.21	0.00	EOP; SAWCUT; MATCH EXISTING
372	76+07.60	78.66 RT	305,365.08	632,465.20	0.00	BEGIN PROFILE GRADE; MATCH EXISTING
373	76+22.48	72.77 RT	305,364.65	632,481.20	0.00	EOP; SAWCUT; MATCH EXISTING
374	76+36.65	77.34 RT	305,354.89	632,492.43	0.00	BOS; SAWCUT; MATCH EXISTING
375	77+27.62	49.97 RT	305,344.34	632,586.85	0.00	R = 95'
376	76+16.20	43.50 RT	305,394.04	632,486.92	1370.80	EOP; BEGIN ASPHALTIC SURFACE
377	76+41.21	43.00 RT	305,384.68	632,510.12	0.00	R = 25'
378	76+21.31	32.50 RT	305,402.15	632,495.94	0.00	BOC; CURB RAMP OPENING
379	76+29.16	32.50 RT	305,399.07	632,503.15	0.00	BOS; FRONT OF SIDEWALK
380	76+34.24	32.50 RT	305,397.07	632,507.83	0.00	BOS
381	76+39.49	32.50 RT	305,395.01	632,512.65	0.00	BOS
382	76+24.90	27.50 RT	305,405.34	632,501.20	0.00	BOC; CURB RAMP OPENING
383		27.49 RT	305,403.67	632,505.11	0.00	FRONT OF SIDEWALK
384	76+29.16	24.00 RT	305,406.88	632,506.49	0.00	BOC; CURB RAMP OPENING
385	76+34.16	21.63 RT	305,407.10	632,512.02	0.00	BOC; CURB RAMP OPENING
386	76+34.16	27.50 RT	305,401.70	632,509.71	0.00	FRONT OF SIDEWALK; BOS
387		18.00 RT	305,407.67	632,519.93	1371.43	EOP; BEGIN RADIUS
388	76+64.49	30.00 RT	305,387.49	632,536.63	1372.26	BOS
389	77+01.91	37.00 LT			1373.48	BOS
390		37.00 LT		632,600.00	1373.48	BOS
391	76+16.21	42.78 RT		632,487.21	1370.80	EOP; END RADIUS
392		22.00 LT		632,356.90	1368.02	PROFILE GRADE BREAK
393	74+42.90	43.76 LT		632,361.80	1368.35	PROFILE GRADE BREAK
394	74+35.86	82.12 LT			0.00	BEGIN PROFILE GRADE; MATCH EXISTING
395	75+76.96	25.40 RT	305,426.09	632,457.94	1370.49	HIGH POINT
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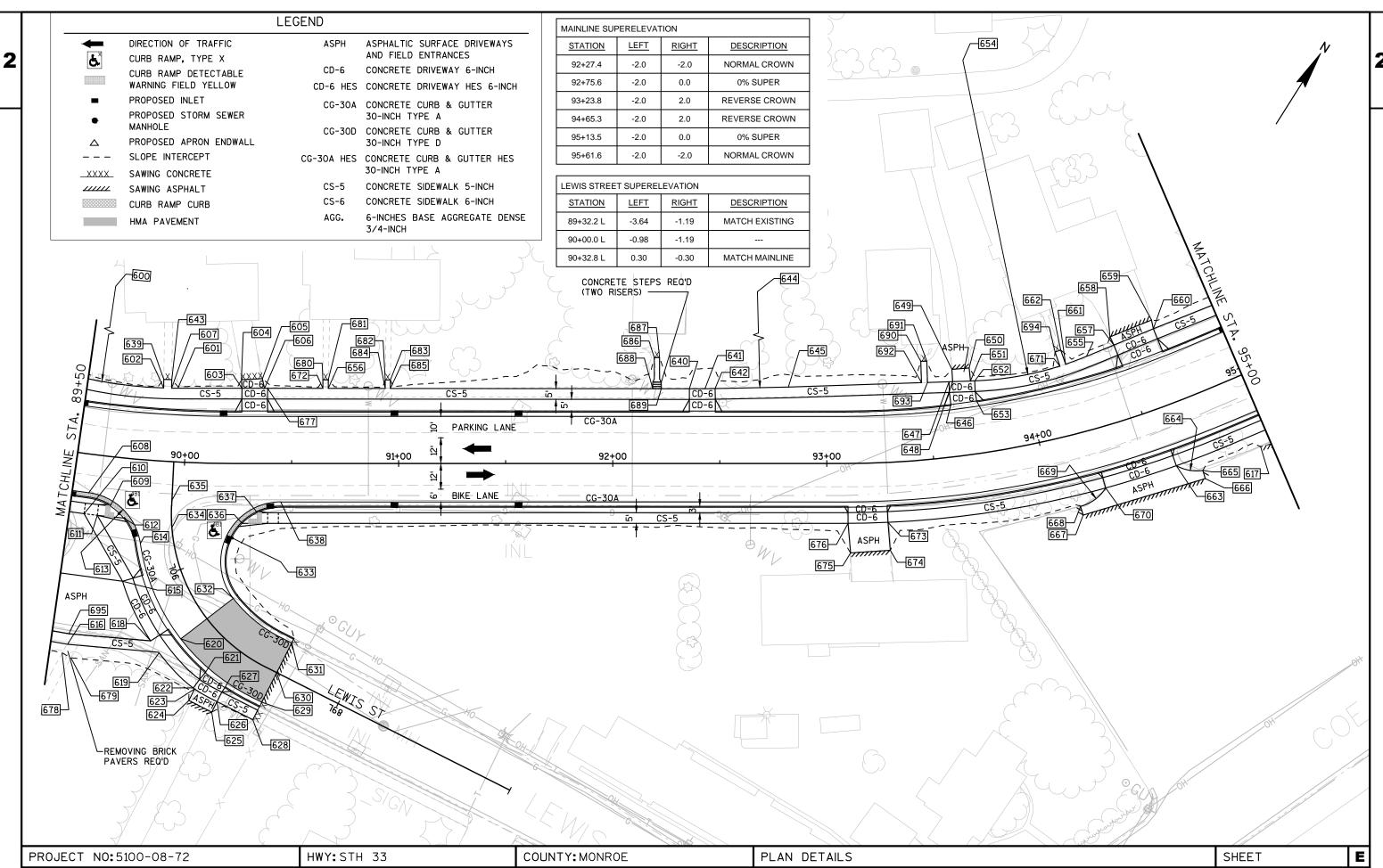
Г					STATION &	OFFSET TAB	LE
D0	INT	STATION	OFFSET	Υ	х		
-		STATION	OFFSET	COORDS	COORDS	ELEVATION	REMARKS
-	00		38.95 LT	305,377.83	632,734.86	0.00	SAWCUT; MATCH EXISTING
-	01		37.00 LT	305,375.88	632,734.46	1371.43	BOS
-	02		36.79 LT	305,354.84		1370.94	BOS CAMOUTA MATCH EXTERTING
\vdash	03	79+02.89	41.04 LT	305,359.21		0.00	SAWCUT; MATCH EXISTING
-	04		36.92 LT		632,745.03	1371.36	BOS; END CONCRETE DRIVEWAY 6-INCH
-	05		36.92 LT	305,371.62		0.00	SAWCUT; MATCH EXISTING
_	06		40.26 LT		632,744.60	0.00	SAWCUT; MATCH EXISTING
	07		38.64 LT			0.00	SAWCUT; MATCH EXISTING
	08		36.85 LT		632,753.78	1371.24	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
	09	79+05.57	41.78 LT			0.00	SAWCUT; MATCH EXISTING
-	10		37.02 LT		632,785.87	1370.93	BOS
-	11		36.74 LT		632,767.53	1371.13	BOS; END CONCRETE DRIVEWAY 6-INCH
	12		39.48 LT	305,364.37		0.00	SAWCUT; MATCH EXISTING
-	13	79+02.04			632,781.27	1370.99	BOS
-	14		44.46 LT		632,902.79	1370.10	BOS
\vdash	15		42.89 LT	305,309.51	632,914.69	0.00	BOS
\vdash	16		22.00 LT			1369.27	EOP; BEGIN RADIUS
\vdash	17		37.00 LT	305,304.11		0.00	R = 15'
-	18		37.38 LT			0.00	BOC; CURB RAMP OPENING
-	19		42.44 LT		632,924.64	0.00	BOC; CURB RAMP OPENING
-	20		44.70 LT		632,927.12	1369.28	EOP; BEGIN ASPHALTIC SURFACE
_	21		61.06 LT		632,926.94	0.00	EOP; SAWCUT; MATCH EXISTING
	22		63.83 LT	305,325.04		0.00	BEGIN PROFILE GRADE; MATCH EXISTING
	23		66.29 LT			0.00	EOP; SAWCUT; MATCH EXISTING
\vdash	24	80+72.52	47.37 LT		632,937.53	1369.39	PROFILE GRADE BREAK
-	25		49.76 LT	305,308.15		1369.33	EOP: BEGIN ASPHALTIC SURFACE
-	26		58.52 LT	305,317.15		1369.63	EOP; END CONCRETE CURB & GUTTER
	27		58.98 LT		632,949.43	1369.84	EOP
\vdash	28	80+87.83	42.21 LT	305,299.84	632,950.12	0.00	BOC; CURB RAMP OPENING
\vdash	29		37.16 LT	305,294.66	632,950.17	0.00	BOC; CURB RAMP OPENING EOP; END RADIUS
-	30					1369.09	
_	31		22.00 LT			1369.00	PROFILE GRADE BREAK
-	32		22.00 LT			1368.80	EOP; BEGIN RADIUS
_	33 34		41.61 LT		632,961.57	0.00	B0S
-	34		37.00 LT			0.00	R = 15'
-	35		41.90 LT 36.04 LT			1369.60	BOS BOS
-	36 37				633,017.03	1368.79	BOS BOS
	38		33.50 LT 22.00 LT	305,279.37	633,052.61	1368.40	BOS EOP; BEGIN RADIUS
-	38 39		37.00 LT		633,052.86	0.00	R = 15'
-	40		28.50 LT	305,274.20	633,061.95	0.00	BOC; CURB RAMP OPENING
-	41		33.50 LT		633,064.84	0.00	BOC; CURB RAMP OPENING
-	42		37.26 LT	305,282.99		1367.31	EOP; END RADIUS; BEGIN ASPHALTIC SURFACE
-	43	82+15.80		305,294.00		0.00	EOP; SAWCUT; MATCH EXISTING
-	43	82+31.80	48.32 LT	305,294.23		0.00	BEGIN PROFILE GRADE; MATCH EXISTING
-	44				633,083.85	1367.34	PROFILE GRADE BREAK
\vdash	45 46		37.31 LT		633,084.09	1367.34	PROFILE GRADE BREAK
-	46		21.12 LT	305,287.04			EOP; SAWCUT; MATCH EXISTING
-	4 / 48	82+47.80	48.37 LT 37.36 LT	305,294.47	633,099.85	0.00 1367.10	EOP; BEGIN ASPHALTIC SURFACE
-	48 49			305,283.45		1367.10	EOP; END RADIUS
	_		36.95 LT O: 5100-		000,000.00		WY'STH 33

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
450	82+62.83	37.00 LT		633,114.85	0.00	R = 15'
451	82+50.83	33.50 LT	305,279.63	633,102.90	0.00	BOC; CURB RAMP OPENING
452	82+53.69	28.48 LT	305,274.65	633,105.81	0.00	BOC; CURB RAMP OPENING
453	82+62.83	22.00 LT	305,268.27	633,115.03	1367.16	EOP; BEGIN RADIUS
454	82+64.38	33.54 LT	305,279.78	633,116.06	0.00	BOS
455			305,281.18		1367.68	BOS
456	82+92.62	34.73 LT	305,281.34	633,144.67	1367.34	BOS
457	83+18.44	34.70 LT	305,281.60	633,170.49	1367.20	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
458	83+18.48	37.29 LT	305,284.19	633,170.50	0.00	SAWCUT; MATCH EXISTING
459			305,284.13		0.00	SAWCUT; MATCH EXISTING
460	83+34.43	34.67 LT	305,281.76	633,186.47	1366.89	BOS; END CONCRETE DRIVEWAY 6-INCH
461			305,281.38		1367.29	BOS
462			305,233.73		1370.29	BOS
463			305,231.12		1370.05	BOS
464		18.00 RT			1369.48	EOP; BEGIN RADIUS
465			305,233.95		0.00	BOC: CURB RAMP OPENING
466		28.00 RT	305,228.30		0.00	BOC; CURB RAMP OPENING
467			305,223.47		1369.68	EOP
468		36.94 RT	305,222.57	·	0.00	R = 15'
469			305,223.04		1369.66	EOP; END RADIUS; END CONCRETE CURB & GUTTER; BEGIN ASPHALTIC SURFACE
470			305,220.00		0.00	EOP; SAWCUT; MATCH EXISTING
471			305,210.00		0.00	BEGIN PROFILE GRADE; MATCH EXISTING
472		32.80 RT	305,220.24		1369.52	
						PROFILE GRADE BREAK
473		18.00 RT			1369.22	PROFILE GRADE BREAK
474		47.05 RT			0.00	EOP; SAWCUT; MATCH EXISTING
475		37.96 RT			0.00	EOP; SAWCUT; MATCH EXISTING
476		33.44 RT			1369.20	EOP
477		33.32 RT			1369.28	EOP; END RADIUS; END CONCRETE CURB & GUTTER; BEGIN ASPHALTIC SURFACE
478			305,222.28		0.00	BOC; CURB RAMP OPENING
479		23.00 RT			0.00	BOC; CURB RAMP OPENING
480			305,230.92 305,216.00			EOP; BEGIN RADIUS
481					0.00	R = 15'
482			305,383.41		1371.89	BOS
483			305,382.09		1371.85	BOS
484			305,387.68		0.00	SAWCUT; MATCH EXISTING
485			305,387.54		0.00	SAWCUT; MATCH EXISTING
486			305,324.78		1370.27	BOS
487			305,327.57		0.00	SAWCUT; MATCH EXISTING
488			305,327.57		0.00	SAWCUT; MATCH EXISTING
489			305,323.46		1370.25	BOS
490			305,282.33		1368.63	BOS
491			305,285.45		0.00	SAWCUT; MATCH EXISTING
492			305,285.53		0.00	SAWCUT; MATCH EXISTING
493			305,281.96		1368.60	BOS
494			305,287.34		0.00	SAWCUT; MATCH EXISTING
495			305,287.38		0.00	SAWCUT; MATCH EXISTING
496			305,376.07		1371.44	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
497		41.17 LT			1369.25	EOP; END RADIUS
498	82+52.24	26.38 LT	305,272.53	633,104.39	1367.22	HIGH POINT



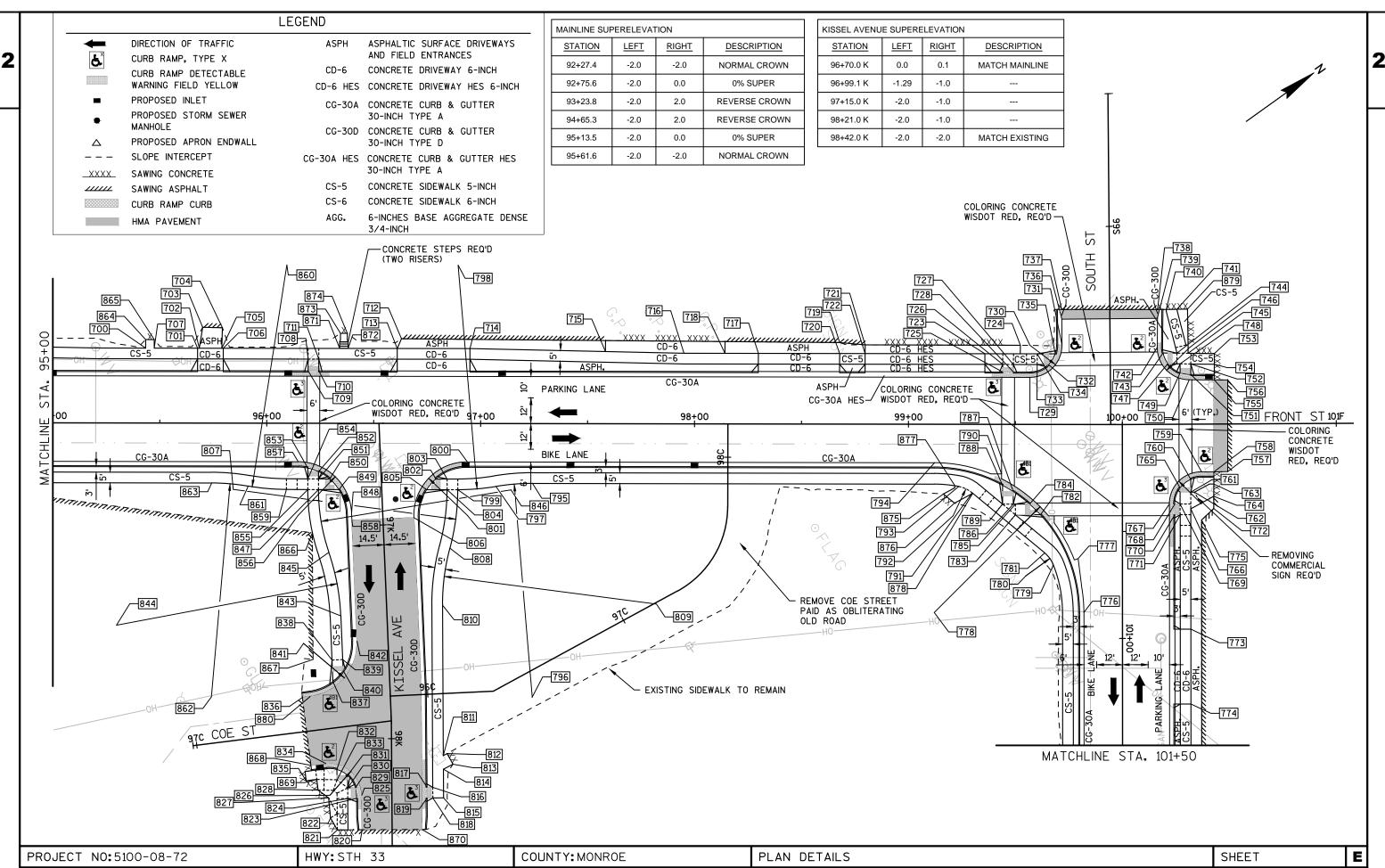
1					STATION & OFF	SET TABLE	
	POINT	STATION	OFFSET	Υ	Х	ELEVATION	REMARKS
				COORDS	COORDS		
	500	84+91.36	34.44 LT	305,283.31	633,343.39	1365.10	BOS
	501	84+07.88	34.56 LT	305,282.49		1366.11	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
	502	84+09.33	34.58 LT	305,282.52			BOS
	503	84+09.23	42.30 LT			0.00	MATCH EXISTING
	504	84+17.88	42.22 LT	305,290.25	633,269.83	0.00	MATCH EXISTING
	505	84+18.75	34.55 LT	305,282.59		1365.95	BOS
	506	84+19.88	34.55 LT	305,282.60	633,271.91	1365.99	BOS; END CONCRETE DRIVEWAY 6-INCH
	507	84+91.37	44.44 LT		633,343.29	0.00	SAWCUT; MATCH EXISTING
	508	84+94.24	44.44 LT	305,293.34	633,346.16	0.00	SAWCUT; MATCH EXISTING
	509	84+52.38	34.50 LT			1365.63	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
	510	84+52.56	38.41 LT	305,286.84		0.00	MATCH EXISTING
	511	84+65.46	38.31 LT	305,286.88	633,317.46	0.00	MATCH EXISTING
	512	84+65.38	34.48 LT		633,317.42	1365.35	BOS; END CONCRETE DRIVEWAY 6-INCH
	513	84+94.22	34.44 LT	305,283.34	633,346.26	1365.08	BOS
	514	85+52.69	34.35 LT	305,283.92		1364.81	BOS
	515	85+23.34	34.39 LT	305,283.63	633,375.37	1364.88	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
	516	85+23.52	43.66 LT	305,292.90	633,375.45	0.00	SAWCUT; MATCH EXISTING
	517	85+34.56	43.53 LT	305,292.90	633,386.48	0.00	SAWCUT; MATCH EXISTING
	518	85+35.34	34.37 LT	305,283.75	633,387.37	1364.76	BOS; END CONCRETE DRIVEWAY 6-INCH
	519	85+52.70	42.03 LT	305,291.60	633,404.65	0.00	SAWCUT; MATCH EXISTING
	520	85+48.94	24.50 LT	305,274.03	633,401.08	0.00	BOC; CURB RAMP OPENING
	521	85+48.94	29.35 LT	305,278.88	633,401.03	0.00	FRONT OF SIDEWALK
	522	85+53.94	29.35 LT	305,278.93	633,406.03	0.00	FRONT OF SIDEWALK
	523	85+53.94	24.50 LT	305,274.09	633,406.08	0.00	BOC; CURB RAMP OPENING
	524	85+55.91	42.03 LT	305,291.64	633,407.86	0.00	SAWCUT; MATCH EXISTING
	525	86+35.26	34.23 LT	305,284.74	633,487.29	1364.41	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
	526	86+35.27	38.48 LT	305,289.00	633,487.25	0.00	MATCH EXISTING
	527	86+60.00	38.21 LT	305,289.00	633,511.98	1363.91	MATCH EXISTING
	528	86+60.26	34.19 LT	305,284.99	633,512.29	1363.92	BOS; END CONCRETE DRIVEWAY 6-INCH
	529	85+55.91	34.34 LT	305,283.95	633,407.94	1364.79	BOS
	530	87+51.37	24.50 LT	305,276.63	633,602.63	0.00	BOC; CURB RAMP OPENING
	531	87+51.53	28.75 LT	305,280.88	633,602.59	0.00	
	532	87+56.82	28.54 LT	305,280.93	633,607.59	0.00	
	533	87+56.63	24.68 LT	305,277.06	633,607.63	0.00	BOC; CURB RAMP OPENING
	534	87+58.91	28.44 LT	305,280.95	633,609.56	0.00	BEGIN RADIUS
	535	87+64.51	33.09 LT	305,285.94	633,614.51	0.00	END RADIUS
	536	87+59.18	33.44 LT	305,285.95	633,609.51	0.00	BOS; R = 5'
	537	87+54.41	22.00 LT	305,274.28	633,605.66	1363.46	EOP; BEGIN RADIUS
	538	87+70.52	36.02 LT	305,289.30	633,619.87	1363.12	EOP; END RADIUS
	539	87+54.41	37.00 LT	305,289.25	633,604.87	0.00	R = 15'
	540	87+67.08	32.91 LT	305,285.94	633,616.92	0.00	BOC; CURB RAMP OPENING
	541	87+67.97	37.86 LT	305,290.93	633,617.37	0.00	BOC; CURB RAMP OPENING
	542	87+64.91	38.08 LT	305,290.94	633,614.52	0.00	
	543	87+71.06	42.10 LT	305,295.40	633,619.85	1363.00	EOP; BEGIN ASPHALTIC SURFACE
	544	87+60.85	52.35 LT	305,304.93	633,609.84	0.00	BOS; SAWCUT; MATCH EXISTING
	545	87+66.05	52.01 LT	305,304.91	633,614.52	0.00	SAWCUT; MATCH EXISTING
	546	87+76.99	100.39 LT	305,353.91	633,619.68	0.00	EOP; SAWCUT; MATCH EXISTING
	547	87+93.02	98.98 LT	305,353.95	633,632.68	0.00	BEGIN PROFILE GRADE; MATCH EXISTING
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POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
548	88+08.92	97.17 LT	305,353.99	633,645.68	0.00	EOP; SAWCUT; MATCH EXISTING
549	87+85.13	40.86 LT	305,295.43	633,632.85	1363.09	PROFILE GRADE BREAK
550	87+82.97	22.00 LT	305,276.47	633,632.91	1363.37	PROFILE GRADE BREAK
551	87+99.12	39.27 LT	305,295.47	633,645.85	1362.97	EOP; END RADIUS; BEGIN ASPHALTIC SURFAC
552	88+02.02	34.30 LT	305,290.93	633,649.22	0.00	BOC; CURB RAMP OPENING
553	88+10.24	33.11 LT	305,290.93	633,656.99	0.00	BOS
554	88+05.04	28.82 LT	305,285.93	633,652.83	0.00	BOC; CURB RAMP OPENING
555	88+10.08	28.07 LT	305,285.93	633,657.65	0.00	
556	88+15.14	22.00 LT	305,280.73	633,663.39	1363.28	EOP; BEGIN RADIUS
557	88+15.14	37.00 LT	305,295.52	633,660.85	0.00	R = 15'
558	88+39.36	35.14 LT	305,298.03	633,683.31	1363.88	BOS
559	89+39.98	35.47 LT	305,327.34	633,772.18	1363.59	BOS
560	89+17.21	58.36 RT	305,231.74	633,786.38	0.00	SAWCUT; MATCH EXISTING
561	89+18.77	84.07 RT	305,208.41	633,797.34	0.00	SAWCUT; MATCH EXISTING
562	89+19.07	89.12 RT	305,203.83	633,799.49	0.00	BOS; SAWCUT; MATCH EXISTING
563	87+54.60	20.50 RT	305,231.84	633,608.08	0.00	BOC; CURB RAMP OPENING
564	87+54.49	23.00 RT	305,229.34	633,608.10	0.00	
565	87+49.70	22.98 RT	305,229.12	633,603.10	0.00	
566	87+49.78	20.50 RT	305,231.60	633,603.08	0.00	BOC; CURB RAMP OPENING
567	85+63.94	28.00 RT	305,221.70	633,416.68	1364.58	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
568	85+98.88	28.00 RT	305,222.10	633,451.62	1364.45	BOS; END CONCRETE DRIVEWAY 6-INCH
569	85+98.94	48.99 RT	305,201.11	633,451.92	0.00	SAWCUT; MATCH EXISTING
570	85+63.94	49.01 RT	305,200.69	633,416.92	0.00	SAWCUT; MATCH EXISTING
571	85+60.44	48.91 RT	305,200.76	633,413.42	0.00	
572	85+63.46	64.97 RT	305,184.73	633,416.62	0.00	SAWCUT; MATCH EXISTING
573	85+55.58	64.58 RT	305,185.03	633,408.74	0.00	SAWCUT; MATCH EXISTING
574	85+55.96	56.98 RT	305,192.63	633,409.03	0.00	END RADIUS
575	85+45.97	56.48 RT	305,193.02	633,399.04	0.00	R = 10'
576	85+55.34	52.85 RT	305,196.76	633,408.37	0.00	BEGIN RADIUS
577	85+48.05	51.44 RT	305,198.08	633,401.06	0.00	SAWCUT; MATCH EXISTING
578	85+48.97	46.69 RT	305,202.84	633,401.92	0.00	SAWCUT; MATCH EXISTING
579	85+54.79	47.82 RT	305,201.78	633,407.76	0.00	
580	85+49.50	32.06 RT	305,217.48	633,402.29	1365.13	BOS
581	85+48.94	28.00 RT	305,221.53	633,401.68	1364.82	BOS
582	85+48.94	23.00 RT	305,226.53	633,401.62	0.00	
583	85+48.94	20.50 RT	305,229.03	633,401.59	0.00	BOC; CURB RAMP OPENING
584	85+53.94	20.50 RT	305,229.09	633,406.59	0.00	BOC; CURB RAMP OPENING
585	85+53.94	23.00 RT	305,226.59	633,406.62	0.00	
586	85+53.94	28.00 RT	305,221.59	633,406.68	1364.79	BOS
587	88+95.99		305,314.53	633,733.33	1363.80	BOS
588	88+96.03		305,318.32		0.00	SAWCUT; MATCH EXISTING
589	89+01.25		305,319.83	633,736.63	0.00	SAWCUT; MATCH EXISTING
590	89+01.16		305,316.03	633,737.88	1363.79	BOS



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POINT	STATION	OFFSET	Υ	х	ELEVATION	REMARKS
	STATION	OI I SEI	COORDS	COORDS	LLLVATION	NEMARKO
L						
600	89+74.47	233.95 LT	305,517.28	633,709.53	0.00	R = 200'
601	89+94.96	34.49 LT	305,349.49	633,818.38	1363.38	BOS; END RADIUS
602	89+87.83	34.18 LT	305,345.96	633,812.73	1363.39	BOS
603	90+26.75	35.16 LT	305,365.69	633,843.51	1363.17	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
604	90+26.54	39.19 LT	305,368.96	633,841.14	0.00	SAWCUT; MATCH EXISTING
605	90+36.35	39.70 LT	305,374.72	633,849.11	0.00	SAWCUT; MATCH EXISTING
606	90+37.32	35.15 LT	305,371.42	633,852.39	1363.17	BOS
607	89+91.82	34.33 LT	305,347.91	633,815.90	1363.38	BOS
608	89+57.28	18.00 RT	305,285.82	633,809.90	1362.92	EOP; BEGIN RADIUS
609	89+67.60	23.00 RT	305,286.06	633,821.74	0.00	BOC; CURB RAMP OPENING
610	89+56.43	28.00 RT	305,276.41	633,813.41	0.00	BOS
611	89+62.34	27.96 RT	305,279.16	633,819.01	0.00	BOS
612	89+74.13	28.00 RT	305,284.73	633,830.12	0.00	BOC; CURB RAMP OPENING
613	89+57.28	46.00 RT	305,260.56	633,821.99	0.00	R = 28'
614	89+82.36	39.15 RT	305,278.98	633,843.08	1362.95	EOP; END RADIUS
615	89+76.16	57.47 RT	305,259.63	633,845.70	1363.68	BOS
616	89+56.56	89.58 RT	305,220.89	633,840.05	0.00	BOS
617	94+95.03	34.41 RT	305,601.14	634,258.11	0.00	SAWCUT: MATCH EXISTING
618	89+89.55	84.21 RT	305,243.36	633,871.82	1363.85	BOS
619	89+93.21	89.53 RT	305,240.81	633,878.16	1363.89	BOS
620	90+01.59	82.52 RT	305,251.78	633,883.12	1363.27	EOP; BEGIN ASPHALTIC SURFACE
621	90+11.78	97.93 RT	305,244.76	633,901.33	0.00	BOC; DRIVEWAY OPENING
622	90+07.23	105.24 RT	305,235.69	633,900.49	1364.02	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
623	90+07.78	105.70 RT	305,235.64	633,901.29	1364.02	BOS
624	90+05.68	111.16 RT	305,229.65	633,901.93	0.00	SAWCUT; MATCH EXISTING
625	90+14.57	116.33 RT	305,230.90	633,913.89	0.00	SAWCUT; MATCH EXISTING
626	90+16.53	111.41 RT	305,236.34	633,913.31	1364.03	BOS; END CONCRETE DRIVEWAY 6-INCH
627	90+19.51	104.83 RT	305,243.83	633,912.83	0.00	BOC; DRIVEWAY OPENING
628	90+31.88	119.81 RT	305,238.33	633,931.94	0.00	BOS; SAWCUT; MATCH EXISTING
629	90+36.01	111.61 RT	305,247.46	633,930.97	0.00	EOP; SAWCUT; MATCH EXISTING
630	90+43.00	97.77 RT	305,262.88	633,929.32	0.00	BEGIN PROFILE GRADE; MATCH EXISTING
631	90+50.01	83.95 RT	305,278.29	633,927.71	0.00	EOP; SAWCUT; MATCH EXISTING
632	90+22.76	63.09 RT	305,281.00	633,893.48	1363.40	EOP; END RADIUS; BEGIN ASPHALTIC SURFACE
633	90+44.92	46.00 RT	305,307.40	633,902.82	0.00	R = 28'
634	89+95.69	30.78 RT	305,293.22	633,851.45	1362.94	PROFILE GRADE BREAK
635	89+95.01	18.00 RT	305,303.96	633,844.48	1362.81	PROFILE GRADE BREAK
636	90+26.85	28.00 RT	305,312.71	633,877.88	0.00	BOC; CURB RAMP OPENING
637	90+33.90	23.00 RT	305,320.73	633,881.09	0.00	BOC; CURB RAMP OPENING
638	90+44.92	18.00 RT	305,330.91	633,887.62	1362.66	EOP; BEGIN RADIUS
639	89+87.59	38.17 LT	305,349.34	633,810.60	0.00	SAWCUT; MATCH EXISTING
640	92+35.85	34.92 LT	305,479.01	634,019.24	1362.67	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
641	92+43.09	34.91 LT	305,482.94	634,025.32	1362.84	BOS; BEGIN RADIUS
642	92+47.84	35.00 LT	305,485.58	634,029.27	1362.82	BOS; END CONCRETE DRIVEWAY 6-INCH
643	89+91.61	38.33 LT	305,351.29	633,813.77	0.00	SAWCUT; MATCH EXISTING
644	62+96.77	3490.24 LT	309,379.16	631,411.61	0.00	R = 4691.7'
645	92+82.08	35.74 LT	305,504.80	634,057.62	1363.68	BOS; END RADIUS
646	93+61.14	24.50 LT	305,539.17	634,125.80	0.00	BOC; DRIVEWAY OPENING
647	93+61.63	35.13 LT	305,547.62	634,119.32	1363.55	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
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POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
648	93+63.41	34.98 LT	305,548.55	634,120.67	1363.41	BOS
649	93+63.86	40.73 LT	305,553.21	634,117.28	0.00	SAWCUT; MATCH EXISTING
650	93+72.77	40.28 LT	305,558.16	634,123.68	0.00	SAWCUT; MATCH EXISTING
651	93+72.43	34.10 LT	305,553.32	634,127.54	1363.22	BOS
652	93+74.62	33.87 LT	305,554.48	634,129.20	1363.52	BOS; BEGIN RADIUS; END CONCRETE DRIVEWAY 6-INC
653	93+74.00	24.50 LT	305,547.09	634,134.99	0.00	BOC; DRIVEWAY OPENING
654	94+04.75	182.23 LT	305,677.36	634,043.18	0.00	R = 150'
655	94+32.55	33.62 LT	305,592.40	634,166.80	1363.14	BOS; END RADIUS
656	90+67.09	35.11 LT	305,387.56	633,877.41	1363.07	BOS
657	94+47.51	34.83 LT	305,603.78	634,174.67	1363.04	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
658	94+46.97	39.05 LT	305,606.03	634,171.07	0.00	SAWCUT; MATCH EXISTING
659	94+70.01		305,623.22			SAWCUT; MATCH EXISTING
660	94+69.25		305,620.23			BOS; END CONCRETE DRIVEWAY 6-INCH
661	94+21.34		305,584.04		1363.08	BOS
662	94+20.85		305,588.40			SAWCUT: MATCH EXISTING
663	94+56.86		305,572.32			BOS; END CONCRETE DRIVEWAY 6-INCH; BEGIN RADI
664	94+71.28		305,592.26			R = 20'
			305,577.17		0.00	SAWCUT; END RADIUS; MATCH EXISTING
665	94+66.99					
666	94+66.71		305,574.62			SAWCUT: MATCH EXISTING
667	94+11.86	37.59 RT		634,205.76	0.00	SAWCUT; MATCH EXISTING
668	94+11.71		305,532.08			SAWCUT; BEGIN RADIUS; MATCH EXISTING
669	94+11.29		305,545.51		0.00	R = 20'
670	94+24.31		305,545.64		1363.95	BOS; END RADIUS
671	94+18.30		305,581.85		1363.11	BOS
672	90+64.58	35.12 LT	305,386.19	633,875.30	1363.09	BOS
673	93+27.33	28.00 RT	305,476.91	634,131.02	1364.15	BOS; END CONCRETE DRIVEWAY 6-INCH
674	93+27.80	41.10 RT	305,466.54	634,139.04	0.00	SAWCUT; MATCH EXISTING
675	93+10.82	41.82 RT	305,455.44	634,124.11	0.00	SAWCUT; MATCH EXISTING
676	93+09.85	28.00 RT	305,466.42	634,115.65	1364.29	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
677	90+38.75	35.15 LT	305,372.20	633,853.59	1363.22	BOS; END CONCRETE DRIVEWAY 6-INCH
678	89+54.93	93.91 RT	305,216.15	633,840.16	0.00	LIMIT OF REMOVING BRICK PAVERS
679	89+58.60	95.63 RT	305,216.47	633,844.84	0.00	LIMIT OF REMOVING BRICK PAVERS
680	90+64.59	39.12 LT	305,389.56	633,873.13	0.00	SAWCUT; MATCH EXISTING
681	90+67.10	39.11 LT	305,390.92	633,875.24	0.00	SAWCUT; MATCH EXISTING
682	90+93.71	39.08 LT	305,405.34	633,897.61	0.00	SAWCUT; MATCH EXISTING
683	90+96.10	39.08 LT	305,406.63	633,899.62	0.00	SAWCUT; MATCH EXISTING
684	90+93.71	35.08 LT	305,401.98	633,899.78	1362.99	BOS
685	90+96.09	35.08 LT	305,403.27	633,901.79	1362.99	BOS
686	92+19.45	49.52 LT	305,482.37	633,997.54	0.00	SAWCUT; MATCH EXISTING
687	92+21.92	49.52 LT	305,483.71	633,999.61	0.00	SAWCUT; MATCH EXISTING
688	92+18.67	34.98 LT	305,469.74	634,004.78	1362.54	BOS
689	92+22.67	34.98 LT			1362.55	BOS
690	93+48.31	46.08 LT			0.00	SAWCUT; MATCH EXISTING
691	93+51.42	45.90 LT			0.00	SAWCUT; MATCH EXISTING
692	93+47.65	36.10 LT			1363.49	BOS
693	93+50.69	35.93 LT		634,111.04	1363.48	BOS
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694	94+17.75	39.54 LT	305,586.21	634,153.36	0.00	SAWCUT; MATCH EXISTING



					STA	TION & OFFSE	T TABLE										
POINT	STATION	OFFSET	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESCRIPTION	POINT	STATION	OFFSET	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESCRIPTION
700	95+43.33	35.00 LT			305,680.13	634,227.92	1361.19	BOS	748			99+64.51 S	32.60 LT	306,076.16	634,505.54	0.00	SEE DETAIL
701	95+67.60	35.00 LT			305,700.19	634,241.58	1361.04	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	749			99+68.74 S	32.57 LT	306,073.79	634,509.04	0.00	BOC; CURB RAMP OPENING
702	95+68.39	35.00 LT			305,700.85	634,242.03	1361.16	BOS	750			99+72.05 S	36.82 LT	306,075.50	634,514.16	1364.85	EOP; END RADIUS
703	95+69.98	44.80 LT			305,707.67	634,234.82	0.00	SAWCUT; MATCH EXISTING	751			99+69.56 S	37.56 LT	306,077.49	634,512.49	0.00	BOC; CURB RAMP OPENING
704	95+79.21	44.59 LT			305,715.18	634,240.19	0.00	SAWCUT; MATCH EXISTING	752			99+64.65 S	37.60 LT	306,080.25	634,508.43	0.00	SEE DETAIL
705	95+78.97	35.00 LT			305,709.59	634,247.98	1361.12	BOS	753			99+59.83 S	48.67 LT	306,092.13	634,510.56	0.00	BOS; SAWCUT; MATCH EXISTING
706	95+79.60	35.00 LT			305,710.11	634,248.34	1361.01	BOS; END CONCRETE DRIVEWAY 6-INCH	754			99+64.83 S	48.60 LT	306,089.30	634,514.68	0.00	SAWCUT; MATCH EXISTING
707	95+47.34	35.00 LT			305,683.45	634,230.18	1361.16	BOS	755			99+72.14 S	48.49 LT	306,085.16	634,520.70	0.00	SAWCUT; MATCH EXISTING; BEGIN ASPHALTIC SURFACE
708	96+19.27	30.00 LT			305,740.08	634,274.80	0.00		756			99+72.25 S	54.82 LT	306,090.36	634,524.30	0.00	SAWCUT; MATCH EXISTING
709	96+19.27	24.50 LT			305,736.99	634,279.34	0.00	BOC; CURB RAMP OPENING	757	100+22.07	49.25 LT			306,066.28	634,559.78	0.00	EOP; MATCH EXISTING
710	96+24.27	24.50 LT			305,741.12	634,282.16	0.00	BOC; CURB RAMP OPENING	758	100+22.18	42.77 LT			306,060.86	634,556.24	0.00	EOP; SAWCUT; BEGIN ASPHALTIC SURFACE
711	96+24.27	30.00 LT			305,744.22	634,277.61	0.00		759	100+22.15	37.06 LT			306,056.15	634,553.02	1365.47	EOP; BEGIN RADIUS
712	96+62.97	39.55 LT			305,781.58	634,291.50	0.00	SAWCUT; MATCH EXISTING	760	100+25.70	32.00 LT			306,049.97	634,553.11	0.00	BOC; CURB RAMP OPENING
713	96+60.96	35.00 LT			305,777.36	634,294.13	1360.78	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	761	100+31.65	32.00 LT			306,046.63	634,558.04	0.00	BOS; SEE DETAIL
714	96+94.96	35.00 LT			305,805.46	634,313.27	1360.78	BOS; END CONCRETE DRIVEWAY 6-INCH	762	100+31.65	33.71 LT			306,048.04	634,559.00	0.00	SEE DETAIL
715	97+58.29	33.73 LT			305,857.09	634,349.96	1361.18	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH BEHIND SIDEWALK	763	100+27.84	42.74 LT			306,057.66	634,560.92	0.00	SAWCUT; MATCH EXISTING
716	97+95.01	33.00 LT			305,887.03	634,371.24	1361.54	BOS	764	100+33.57	42.72 LT			306,054.42	634,565.65	0.00	BOS; SAWCUT; MATCH EXISTING
717	98+29.78	33.00 LT			305,915.77	634,390.81	1362.04	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	765	100+29.65	27.00 LT			306,043.61	634,553.58	0.00	BOC; CURB RAMP OPENING
718	98+14.15	33.00 LT			305,902.85	634,382.01	1361.76	BOS; END CONCRETE DRIVEWAY 6-INCH	766	100+39.42	32.00 LT			306,042.27	634,564.48	0.00	BOS; SEE DETAIL
719	98+67.78	38.12 LT			305,950.06	634,407.97	0.00	SAWCUT; MATCH EXISTING	767	100+37.15	22.00 LT			306,035.26	634,556.99	1365.80	EOP; END RADIUS
720	98+67.78	33.00 LT			305,947.18	634,412.19	1362.53	BOS; END CONCRETE DRIVEWAY 6-INCH	768	100+37.15	24.50 LT			306,037.33	634,558.39	0.00	BOC; CURB RAMP OPENING
721	98+79.69	37.03 LT			305,959.29	634,415.56	0.00	SAWCUT; MATCH EXISTING	769	100+37.15	27.00 LT			306,039.40	634,559.79	0.00	SEE DETAIL
722	98+79.68	33.00 LT			305,957.01	634,418.89	1362.91	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	770	100+42.15	24.50 LT			306,034.53	634,562.53	0.00	BOC; CURB RAMP OPENING
723	99+35.68	33.00 LT			306,003.30	634,450.41	1364.44	BOS; END CONCRETE DRIVEWAY 6-INCH	771	100+42.15	27.00 LT			306,036.60	634,563.93	0.00	SEE DETAIL
724	99+52.25	36.99 LT			306,019.24	634,456.44	0.00	SAWCUT; MATCH EXISTING	772	100+39.28	42.70 LT			306,051.20	634,570.36	0.00	SAWCUT; MATCH EXISTING
725	99+44.05	24.50 LT			306,005.43	634,462.15	0.00	BOC; CURB RAMP OPENING	773	100+95.73	32.00 LT			306,010.68	634,611.09	1366.58	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
726	99+44.05	28.00 LT			306,007.40	634,459.25	0.00	FRONT OF SIDEWALK; SEE DETAIL	774	101+30.73	32.00 LT			305,991.05	634,640.07	1366.11	BOS; END CONCRETE DRIVEWAY 6-INCH
727	99+49.05	28.00 LT			306,011.54	634,462.07	0.00	FRONT OF SIDEWALK; SEE DETAIL	775	100+44.67	37.00 LT			306,043.46	634,571.62	0.00	SAWCUT; MATCH EXISTING
728	99+49.05	24.50 LT			306,009.57	634,464.96	0.00	BOC; CURB RAMP OPENING	776	100+87.80	18.00 RT			305,973.74	634,576.48	1366.17	EOP; END RADIUS
729	99+56.74	22.00 LT			306,014.52	634,471.36	1364.49	EOP; BEGIN RADIUS	777	100+62.65	20.73 RT			305,985.58	634,554.13	1365.87	EOP
730	99+56.74	37.00 LT			306,022.96	634,458.96	0.00	R = 15'	778	100+87.80	88.00 RT			305,915.79	634,537.21	0.00	R = 70'
731	99+71.74	37.15 LT			306,035.45	634,467.28	1364.26	EOP; END RADIUS	779	100+65.62	32.25 RT			305,974.38	634,550.12	1366.34	BOS; END RADIUS
732	99+65.42	28.00 LT			306,025.07	634,471.28	0.00	BOC; CURB RAMP OPENING	780	100+70.79	45.26 RT			305,960.71	634,547.11	0.00	R = 14'
733	99+60.13	28.00 LT			306,020.69	634,468.30	0.00	FRONT OF SIDEWALK; SEE DETAIL	781	100+60.02	36.32 RT			305,974.16	634,543.20	1366.14	BOS; BEGIN RADIUS
734	99+60.13	33.00 LT			306,023.51	634,464.17	0.00	BOS; SEE DETAIL	782	100+42.15	38.27 RT			305,982.56	634,527.32	0.00	BOC; CURB RAMP OPENING
735	99+68.59	33.00 LT			306,030.50	634,468.93	0.00	BOC; CURB RAMP OPENING	783	99+50.08	42.27 RT			305,972.84	634,520.73	0.00	SEE DETAIL
736	99+71.62	49.20 LT			306,042.13	634,457.25	0.00	EOP; BEGIN ASPHALTIC SURFACE	784	99+56.71	37.25 RT			305,981.14	634,520.31	0.00	BOC; CURB RAMP OPENING
737	99+71.57	53.71 LT			306,044.63	634,453.50	0.00	EOP; MATCH EXISTING	785	99+50.07	37.27 RT			305,975.65	634,516.59	0.00	SEE DETAIL
738			99+39.04 S 2	22.31 LT	306,081.72	634,478.64	0.00	EOP; MATCH EXISTING	786	99+49.05	37.27 RT			305,974.80	634,516.02	0.00	SEE DETAIL
739			99+43.18 S 2				1364.10	EOP; BEGIN ASPHALTIC SURFACE	787	99+49.05	31.44 RT			305,978.08	634,511.20	0.00	BOC; CURB RAMP OPENING
740			99+39.06 S 2				0.00	SAWCUT; MATCH EXISTING	788	99+44.05	28.48 RT			305,975.61	634,505.94	0.00	BOC; CURB RAMP OPENING
741			99+39.02 S 3	31.27 LT	306,089.19	634,483.59	0.00	BOS; SAWCUT; MATCH EXISTING	789	99+44.05	37.28 RT			305,970.66	634,513.21	0.00	SEE DETAIL
742			99+56.79 S 2				1364.66	EOP; BEGIN RADIUS	790	99+43.55	23.18 RT			305,978.18	634,501.28	1364.37	EOP
743			99+59.40 S 2	24.73 LT	306,072.44	634,496.93	0.00	BOC; CURB RAMP OPENING	791	99+29.86	32.04 RT			305,961.88	634,500.90	1364.56	BOS; END RADIUS
744			99+59.48 S 3	31.17 LT	306,077.76	634,500.56	0.00	SEE DETAIL	792	99+20.94	42.84 RT			305,948.43	634,504.80	0.00	R = 14'
745			99+59.61 S 3				0.00	BOS; SEE DETAIL	793	99+23.60	29.09 RT			305,958.37	634,494.93	1364.31	BOS; BEGIN RADIUS
746			99+57.05 S 3				0.00	R = 15'	794	99+12.20	18.00 RT			305,955.19	634,479.35	1363.39	EOP; BEGIN RADIUS
747			99+64.43 S 2	26.91 LT	306,071.47	634,502.32	0.00	BOC; CURB RAMP OPENING									

FILE NAME: S:\MAD\1000--1099\1089\837\ACAD\CIVIL 3D\SHEETSPLAN\021201_PD.DWG LAYOUT NAME - 021201_PD - 021207A_PD

HWY:STH 33

PROJECT NO:5100-08-72

COUNTY: MONROE

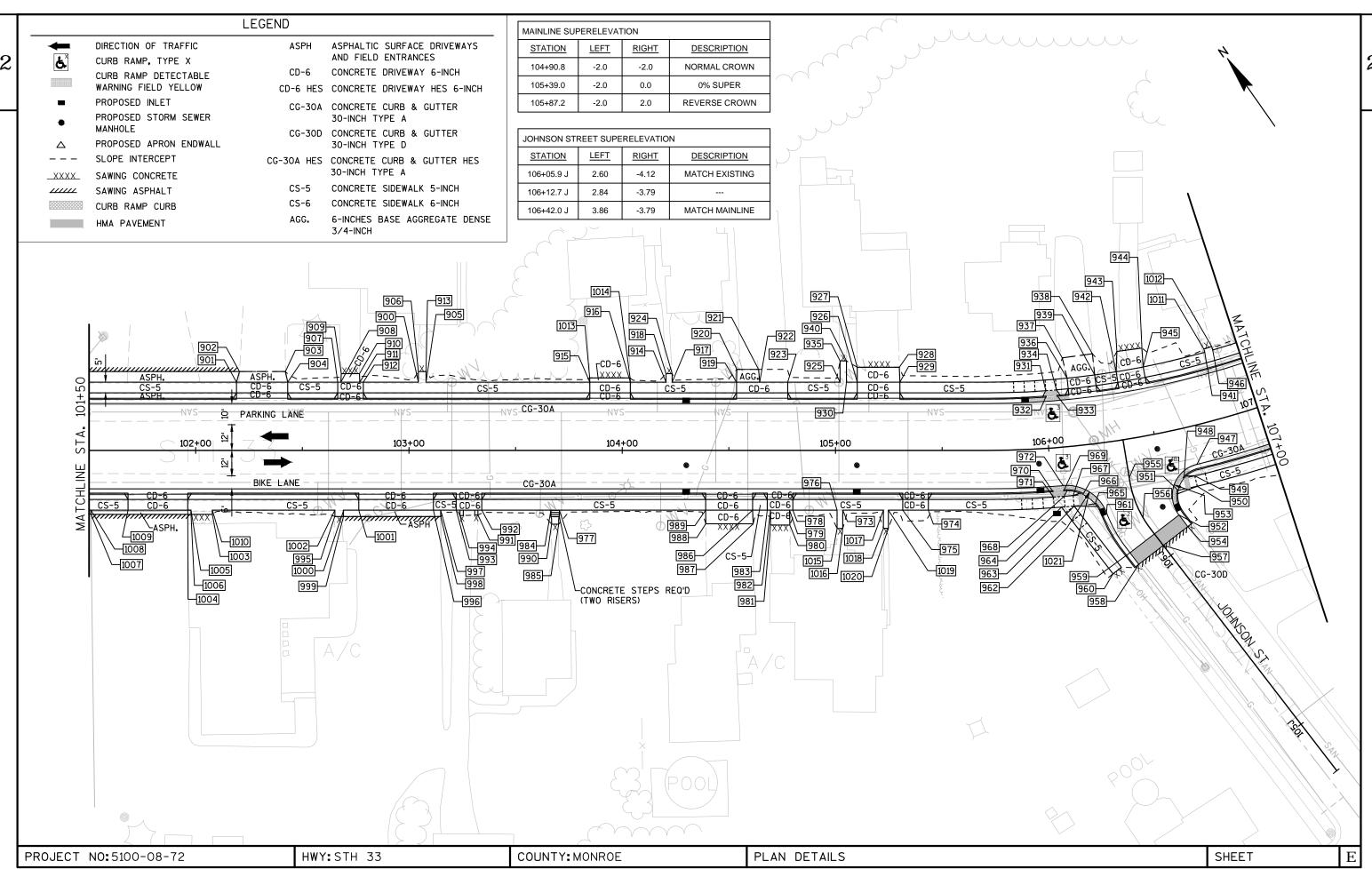
PLAN DETAILS

E

SHEET

J						STATION &	OFFSET TABL	E	
Ì	POINT	STATION	OFFSET	STATION	OFFSET	Υ	х	ELEVATION	DESCRIPTION
ŀ				01/112011	011021	COORDS	COORDS		
ŀ	795 796	97+24.30	28.00 RT 128.00 RT			305,794.25	634,381.86		BOS; END RADIUS R = 100'
ŀ	797	97+07.26	29.46 RT				634,373.47		BOS; BEGIN RADIUS; END RADIUS
ł	798	96+89.36	74.00 LT				634,277.88		R = 105'
1	799	96+89.36	31.00 RT				634,364.67		BOS; BEGIN RADIUS
ł	800	96+93.85	18.00 RT				634,356.45		EOP: BEGIN RADIUS
ł	801	96+93.85	43.00 RT				634,377.11	0.00	R = 25'
ŀ	802	96+79.11	26.00 RT			305,758.03			BOC: CURB RAMP OPENING
ł	803	96+84.36	26.00 RT				634,357.72		FRONT OF SIDEWALK
ł	804	96+84.36	31.00 RT			305,759.55		0.00	BOS
ł	805	96+74.82	31.00 RT				634,356.48		BOC; CURB RAMP OPENING
ł	806	96+68.88		96+97.20 K	14.50 LT		634,363.95		EOP; END RADIUS; BEGIN ASPHALTIC SURFACE
ł	807	95+84.36		96+80.49 K		305,676.89		0.00	R = 105'
ł	808	96+85.75		97+12.14 K		305,745.33			BOS; END RADIUS
ł	809	97+82.31	84.30 RT	97+42.28 K			634,461.04		R = 100'
ł	810	96+82.31	84.99 RT	97+38.65 K			634,405.32		BOS; END RADIUS
ł	811		155.31 RT	98+08.11 K			634,463.72		BOS
Ì	812	96+84.56	154.34 RT	98+07.15 K	25.96 LT	305,690.29	634,463.91	0.00	SAWCUT; MATCH EXISTING
Ì	813	96+87.30	159.34 RT	98+12.18 K		305,689.74		0.00	SAWCUT; MATCH EXISTING
Ì	814	96+82.84	161.64 RT	98+14.44 K	24.18 LT	305,684.76	634,468.98	1365.46	BOS
Ì	815	96+82.93	175.06 RT	98+27.86 K	24.15 LT	305,677.28	634,480.12	0.00	BOS
Ì	816	96+77.89	170.10 RT	98+22.86 K	19.16 LT	305,675.91	634,473.19	0.00	FRONT OF SIDEWALK
Ì	817	96+75.23	170.13 RT	98+22.86 K	16.50 LT	305,673.70	634,471.71	0.00	BOC; CURB RAMP OPENING
İ	818	96+77.93	175.10 RT	98+27.86 K	19.15 LT	305,673.12	634,477.34	0.00	FRONT OF SIDEWALK
Ì	819	96+75.28	175.13 RT	98+27.86 K	16.50 LT	305,670.92	634,475.87	0.00	BOC; CURB RAMP OPENING
İ	820	96+44.91	190.05 RT	98+42.52 K	14.00 RT	305,637.41	634,471.11	0.00	EOP; SAWCUT; MATCH EXISTING
İ	821	96+33.29	190.14 RT	98+42.51 K	25.62 RT	305,627.76	634,464.64	0.00	BOS; SAWCUT; MATCH EXISTING
Ī	822	96+30.13	184.56 RT	98+36.90 K	28.73 RT	305,628.29	634,458.26	0.00	BOS; SAWCUT; MATCH EXISTING
Ī	823	96+42.28	175.41 RT	98+27.86 K	16.50 RT	305,643.48	634,457.53	0.00	BOC; CURB RAMP OPENING
Ī	824	96+38.18	175.45 RT	98+27.86 K	20.60 RT	305,640.07	634,455.25	0.00	FRONT OF SIDEWALK; SEE DETAIL
Ī	825	96+44.77	173.84 RT	98+26.31 K	14.00 RT	305,646.42	634,457.63	1364.90	EOP; END RADIUS
	826	96+29.77	173.97 RT	98+26.31 K	29.00 RT	305,633.95	634,449.30	0.00	R = 15'
	827	96+29.19	175.12 RT	98+27.45 K	29.59 RT	305,632.83	634,449.92	0.00	BOS; SAWCUT; MATCH EXISTING
	828	96+24.26	171.54 RT	98+23.83 K	34.48 RT	305,630.77	634,444.19	0.00	BOS; SAWCUT; MATCH EXISTING
	829	96+41.75	170.42 RT	98+22.86 K	16.98 RT	305,645.86	634,453.11	0.00	BOC; CURB RAMP OPENING
	830	96+38.14	170.45 RT	98+22.86 K	20.59 RT	305,642.86	634,451.10	0.00	FRONT OF SIDEWALK; SEE DETAIL
	831	96+38.10	164.65 RT	98+17.06 K	20.59 RT	305,646.08	634,446.29	0.00	BOC; CURB RAMP OPENING
l	832	96+33.08	161.92 RT	98+14.28 K	25.58 RT	305,643.47	634,441.20	0.00	BOC; CURB RAMP OPENING
ı	833	96+33.11	166.69 RT	98+19.06 K	25.59 RT	305,640.82	634,445.16	0.00	FRONT OF SIDEWALK; SEE DETAIL
I	834	96+27.87	159.09 RT	98+11.41 K	30.76 RT	305,640.76	634,435.93	1365.16	EOP; BEGIN RADIUS
	835	96+18.70	167.16 RT	98+19.40 K	40.01 RT	305,628.63	634,437.44	0.00	BOS; SAWCUT; MATCH EXISTING
	836	96+16.90	128.23 RT	97+79.03 K	41.05 RT	305,649.07	634,404.25	0.00	EOP; SAWCUT; MATCH EXISTING
	837	96+31.15	120.75 RT	97+72.18 K	26.50 RT	305,665.05	634,406.09	0.00	BOC; CURB RAMP OPENING
	838	96+30.71	110.72 RT	97+62.13 K	26.50 RT	305,670.34	634,397.55	0.00	BOS
	839	96+35.71	110.50 RT	97+62.13 K	21.50 RT	305,674.59	634,400.18	0.00	FRONT OF SIDEWALK
	840	96+35.93	115.68 RT	97+67.32 K	21.50 RT	305,671.86	634,404.59	0.00	BOC; CURB RAMP OPENING
ļ	841		102.97 RT		39.50 RT	305,663.67		0.00	R = 25'
ļ	842	96+42.34	101.89 RT	97+53.82 K	14.50 RT	305,684.92	634,396.80	1363.47	EOP; BEGIN RADIUS

POINT	STATION	OFFSET	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESCRIPTION
843	96+34.83	90.04 RT	97+41.66 K	21.50 RT	305,685.37	634,382.77	0.00	FRONT OF SIDEWALK; END RADIUS
844	95+29.92	94.57 RT	97+41.66 K	126.50 RT	305,596.12	634,327.47	0.00	R = 105'
845	96+29.19	60.34 RT	97+11.74 K	25.85 RT	305,697.43	634,355.05	0.00	FRONT OF SIDEWALK; END RADIUS; BEGIN RADIUS
846	97+14.27	31.00 RT	96+86.10 K	60.42 LT	305,784.27	634,378.69	0.00	R = 90'
847	96+24.27	31.00 RT			305,709.88	634,328.03	0.00	BOS
848	96+39.76	41.92 RT			305,716.53	634,345.77	1360.85	EOP; END RADIUS
849	96+33.81	31.00 RT			305,717.77	634,333.40	0.00	BOC; CURB RAMP OPENING
850	96+29.52	26.00 RT			305,717.03	634,326.85	0.00	BOC; CURB RAMP OPENING
851	96+24.27	26.00 RT			305,712.70	634,323.90	0.00	FRONT OF SIDEWALK
852	96+24.27	22.60 RT			305,714.61	634,321.09	0.00	BOC; CURB RAMP OPENING
853	96+19.27	20.95 RT			305,711.40	634,316.91	0.00	BOC; CURB RAMP OPENING
854	96+19.27	26.00 RT			305,708.56	634,321.08	0.00	FRONT OF SIDEWALK
855	96+19.27	31.00 RT			305,705.75	634,325.22	0.00	BOS
856	96+14.78	43.00 RT			305,695.28	634,332.61	0.00	R = 25'
857	96+14.78	18.00 RT			305,709.35	634,311.94	1361.13	EOP; BEGIN RADIUS
858	96+39.90	45.33 RT			305,714.74	634,348.67	1360.88	EOP; BEGIN ASPHALTIC SURFACE
859	96+09.27	31.00 RT			305,697.48	634,319.59	1361.82	BOS; END RADIUS
860	96+09.27	74.00 LT			305,756.58	634,232.80	0.00	R = 105'
861	95+91.37	29.46 RT			305,683.56	634,308.24	1361.83	BOS; END RADIUS; BEGIN RADIUS
862	95+74.33	128.00 RT			305,614.00	634,380.10	0.00	R = 100'
863	95+74.33	28.00 RT			305,670.29	634,297.44	1361.86	BOS; BEGIN RADIUS
864	95+43.33	39.00 LT			305,682.38	634,224.61	0.00	SAWCUT; MATCH EXISTING
865	95+47.34	39.00 LT			305,685.70	634,226.87	0.00	SAWCUT; MATCH EXISTING
866	96+21.41	51.22 RT			305,696.14	634,343.13	0.00	SAWCUT; MATCH EXISTING
867	96+21.85	110.40 RT	97+61.43 K	35.35 RT	305,663.19	634,392.29	0.00	SAWCUT; MATCH EXISTING
868	96+20.94	159.97 RT	98+12.23 K	37.71 RT	305,634.53	634,432.76	0.00	EOP; SAWCUT; MATCH EXISTING
869	96+23.66	166.53 RT	98+18.81 K	35.05 RT	305,633.09	634,439.71	0.00	BOS; SAWCUT; MATCH EXISTING
870	96+72.91	189.85 RT	98+42.56 K	14.00 LT	305,660.67	634,486.70	0.00	EOP; SAWCUT; MATCH EXISTING
871	96+34.01	35.04 LT			305,755.10	634,278.93	1360.75	BOS
872	96+38.01	35.04 LT			305,758.41	634,281.18	1360.75	BOS
873	96+34.43	42.08 LT			305,759.41	634,273.34	0.00	SAWCUT; MATCH EXISTING
874	96+37.59	42.08 LT			305,762.03	634,275.12	0.00	SAWCUT; MATCH EXISTING
875	99+16.37	32.60 RT			305,950.42	634,493.76	0.00	SAWCUT; MATCH EXISTING
876	99+21.56	36.89 RT			305,952.29	634,500.23	0.00	SAWCUT; MATCH EXISTING
877	99+21.80	28.77 RT			305,957.06	634,493.66	1364.25	BOS
878	99+30.13	32.27 RT			305,961.98	634,501.24	1364.58	BOS
879			99+38.97 S	36.29 LT	306,093.39	634,486.33	0.00	SAWCUT; MATCH EXISTING
880	96+20.52	127.77 RT	97+78.73 K	37.42 RT	305,652.32	634,405.90	1365.27	EOP; END RADIUS



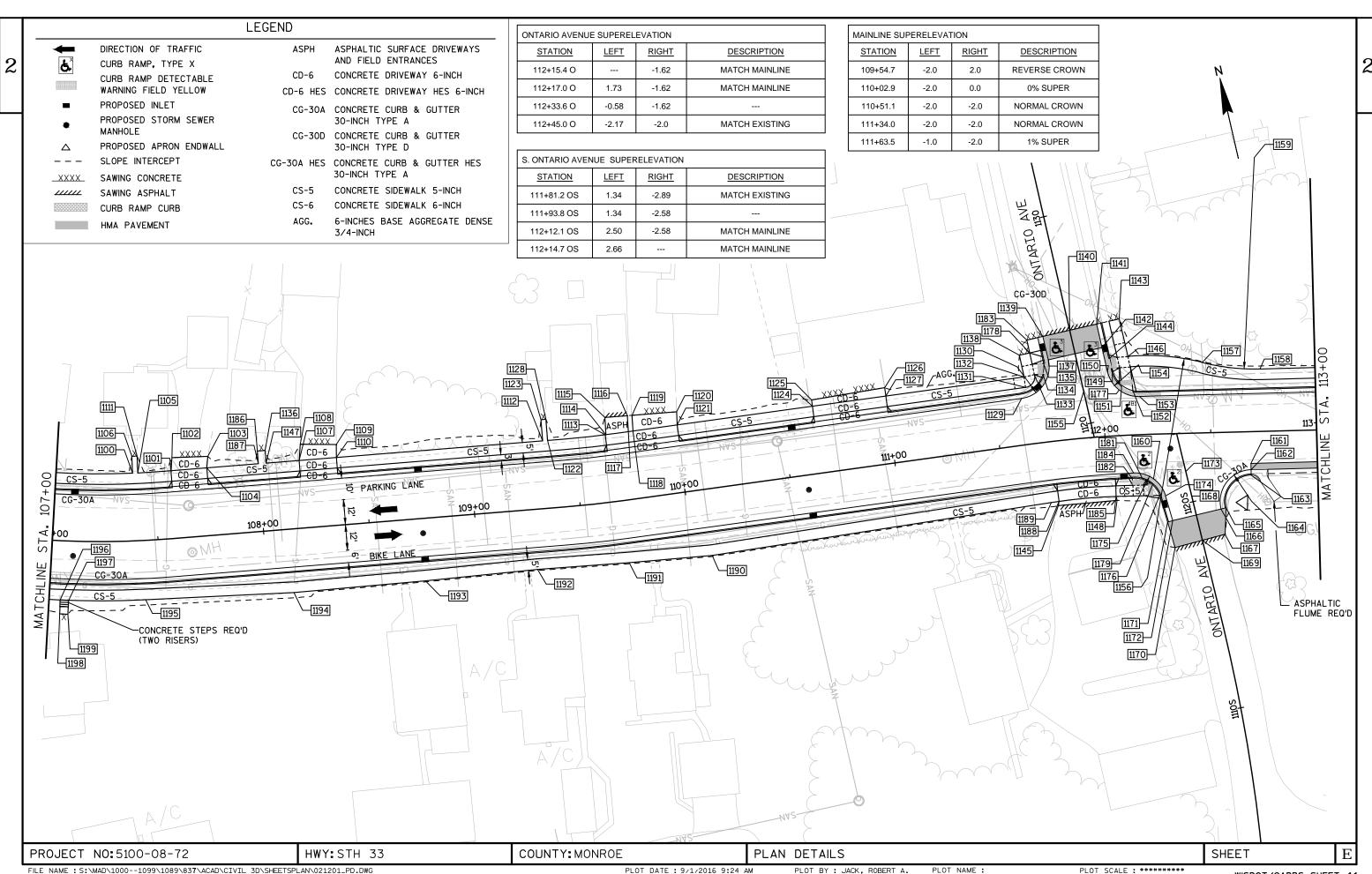
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			:	STATION & OFF	SET TABLE	
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
900	103+04.42	32.00 LT	305,893.62	634,783.85	1361.84	BOS
901	102+19.19	32.00 LT	305,941.43	634,713.29	1363.27	BOS; BEGIN CONCRETE DRIVEWAY 6-INC
902	102+20.59	37.00 LT	305,944.78	634,717.26	0.00	SAWCUT; MATCH EXISTING
903	102+42.09	37.00 LT	305,932.72	634,735.06	0.00	MATCH EXISTING
904	102+43.19	32.00 LT	305,927.96	634,733.16	1362.58	BOS; END CONCRETE DRIVEWAY 6-INCH
905	103+08.06	32.00 LT	305,891.57	634,786.87	1361.66	BOS
906	103+04.42	37.00 LT	305,897.76	634,786.66	0.00	SAWCUT; MATCH EXISTING
907	102+66.62	32.00 LT	305,914.82	634,752.56	1362.05	BOS; BEGIN CONCRETE DRIVEWAY 6-INC
908	102+68.12	32.00 LT	305,913.98	634,753.80	1361.92	BOS
909	102+68.32	36.00 LT	305,917.18	634,756.21	0.00	SAWCUT; MATCH EXISTING
910	102+76.84	35.99 LT	305,912.39	634,763.26	0.00	SAWCUT; MATCH EXISTING
911	102+76.97	32.00 LT	305,909.02	634,761.13	1361.80	BOS
912	102+78.62	32.00 LT	305,908.09	634,762.49	1361.86	BOS; END CONCRETE DRIVEWAY 6-INCH
913	103+08.06	37.00 LT	305,895.71	634,789.67	0.00	SAWCUT; MATCH EXISTING
914	104+20.56	32.00 LT	305,828.47	634,880.00	1359.06	BOS
915	103+84.82	32.00 LT	305,848.51	634,850.42	1360.07	BOS; BEGIN CONCRETE DRIVEWAY 6-INC
916	104+03.82	32.00 LT	305,837.85	634,866.14	1359.47	BOS; END CONCRETE DRIVEWAY 6-INCH
917	104+23.41	32.00 LT	305,826.87	634,882.36	1358.97	BOS
918	104+20.56	36.00 LT	305,831.78	634,882.24	0.00	SAWCUT; MATCH EXISTING
919	104+53.62	32.00 LT	305,809.92	634,907.37	1357.71	BOS; BEGIN CONCRETE DRIVEWAY 6-INC
920	104+53.76	38.00 LT	305,814.81	634,910.85	0.00	MATCH EXISTING
921	104+63.93	38.00 LT	305,809.10	634,919.27	0.00	MATCH EXISTING
922	104+64.47	32.00 LT	305,803.83	634,916.35	1357.13	BOS
923	104+77.62	32.00 LT	305,796.46	634,927.24	1356.64	BOS; END CONCRETE DRIVEWAY 6-INCH
924	104+23.41	36.00 LT	305,830.18	634,884.60	0.00	SAWCUT; MATCH EXISTING
925	105+02.14	32.00 LT	305,782.71	634,947.53	1355.89	BOS
926	105+10.21	32.00 LT	305,778.17	634,954.22	1355.57	BOS; BEGIN CONCRETE DRIVEWAY 6-INC
927	105+10.19	39.00 LT	305,783.98	634,958.13	0.00	SAWCUT; MATCH EXISTING
928	105+29.98	39.00 LT	305,772.88	634,974.51	0.00	SAWCUT; MATCH EXISTING
929	105+30.24	32.00 LT	305,766.94	634,970.80	1354.69	BOS; END CONCRETE DRIVEWAY 6-INCH
930	105+05.15	32.00 LT	305,781.02	634,950.02	1355.77	BOS
931	106+00.58	27.00 LT	305,725.26	635,025.15	0.00	FRONT OF SIDEWALK
932	106+00.59	24.54 LT	305,723.12	635,023.93	0.00	BOC; CURB RAMP OPENING
933	106+05.91	24.50 LT	305,720.62	635,028.26	0.00	BOC; CURB RAMP OPENING
934	106+05.93	27.00 LT	305,722.79	635,029.50	0.00	FRONT OF SIDEWALK
935	105+02.14	42.00 LT	305,790.98	634,953.14	0.00	SAWCUT; MATCH EXISTING
936	106+11.87	32.00 LT	305,724.51	635,036.74	1351.45	BOS; BEGIN CONCRETE DRIVEWAY 6-INC
937	106+10.40	41.72 LT	305,733.70	635,040.21	0.00	MATCH EXISTING
938	106+26.28	42.00 LT	305,727.35	635,052.98	0.00	MATCH EXISTING
939	106+25.98	32.00 LT	305,718.52	635,048.28	1350.90	BOS; END CONCRETE DRIVEWAY 6-INCH
940	105+05.15	42.00 LT	305,789.30	634,955.63	0.00	SAWCUT; MATCH EXISTING
941	106+83.19	32.00 LT	305,698.39	635,096.99	1348.68	BOS
942	106+37.06	32.00 LT	305,714.09	635,057.48	1350.48	BOS; BEGIN CONCRETE DRIVEWAY 6-INC
943	106+39.08	41.38 LT	305,721.84	635,063.09	0.00	SAWCUT; MATCH EXISTING
944	106+52.26	40.80 LT	305,716.54	635,073.71	0.00	SAWCUT; MATCH EXISTING

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
945	106+50.97	32.00 LT	305,708.89	635,069.21	1349.91	BOS; END CONCRETE DRIVEWAY 6-INCH
946	106+87.41	32.00 LT	305,697.17	635,100.68	1348.52	BOS
947	106+66.20	18.00 RT	305,656.92	635,064.45	1349.61	EOP; END RADIUS
948	106+59.18	23.00 RT	305,654.94	635,055.77	0.00	BOC; CURB RAMP OPENING
949	106+64.11	23.00 RT	305,653.04	635,060.61	0.00	FRONT OF SIDEWALK
950	106+64.11	28.00 RT	305,648.38	635,058.81	0.00	BOS
951	106+55.54	28.00 RT	305,651.76	635,050.30	0.00	BOC; CURB RAMP OPENING
952	106+53.73	39.38 RT	305,641.99	635,044.15	1350.88	EOP; BEGIN RADIUS
953	106+66.20	33.00 RT	305,642.90	635,059.12	0.00	R = 15'
954	106+56.43	45.53 RT	305,635.18	635,044.56	0.00	SAWCUT; MATCH EXISTING
955	106+35.22	18.00 RT	305,669.56	635,034.69	1350.84	PROFILE GRADE BREAK
956	106+38.38	37.53 RT	305,650.43	635,029.51	1351.14	PROFILE GRADE BREAK
957	106+44.83	51.99 RT	305,634.30	635,030.09	0.00	MATCH EXISTING; BEGIN PROFILE GRADE
958	106+33.61	58.78 RT	305,633.43	635,015.69	0.00	EOP; MATCH EXISTING
959	106+30.64	52.85 RT	305,640.24	635,015.20	1351.84	EOP; BEGIN ASPHALTIC SURFACE
960	106+22.58	61.59 RT	305,636.49	635,003.09	0.00	BOS; SAWCUT; MATCH EXISTING
961	106+23.10	32.86 RT	305,661.85	635,016.61	1351.52	EOP: END RADIUS
962	106+05.31	38.00 RT	305,666.42	634,997.14	0.00	R = 20'
963	106+09.37	28.00 RT	305,673.03	635,005.83	0.00	BOS; SEE DETAIL
964	106+03.80	28.00 RT	305,675.92	635,000.64	0.00	BOS; SEE DETAIL
965	106+18.60	28.00 RT	305,668.38	635,014.53	0.00	BOC; CURB RAMP OPENING
966	106+13.70	23.00 RT	305,675.23	635,012.26	0.00	BOC; CURB RAMP OPENING
967	106+09.37	23.00 RT	305,677.41	635,008.23	0.00	FRONT OF SIDEWALK; SEE DETAIL
968	106+05.33	23.00 RT	305,679.48	635,004.50	0.00	FRONT OF SIDEWALK; SEE DETAIL
969	106+05.35	20.50 RT	305,681.65	635,005.75	0.00	BOC; CURB RAMP OPENING
970	106+00.59	20.50 RT	305,684.12	635,001.41	0.00	BOC: CURB RAMP OPENING
971	106+00.59	23.00 RT	305,681.96	635,000.16	0.00	FRONT OF SIDEWALK; SEE DETAIL
972	106+05.31	18.00 RT	305,683.85	635,006.94	1352.03	EOP; BEGIN RADIUS
973	105+03.42	28.00 RT	305,732.31	634,914.94	1356.32	BOS
974	105+43.58	28.00 RT	305,709.79	634,948.18	1354.59	BOS; END CONCRETE DRIVEWAY 6-INCH
975	105+31.58	28.00 RT	305,716.52	634,938.25	1354.88	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
976	105+00.78	28.00 RT	305,733.80	634,912.75	1356.42	BOS
977	103+70.55	28.04 RT	305,806.81	634,804.92	1360.53	BOS
978	104+80.05	28.00 RT	305,745.43	634,895.59	1356.69	BOS; END CONCRETE DRIVEWAY 6-INCH
979	104+78.50	28.00 RT	305,746.29	634,894.31	1356.65	BOS

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			•	STATION & OFF	SET TABLE	
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
980	104+78.60	35.00 RT	305,740.44	634,890.47	0.00	SAWCUT; MATCH EXISTING
981	104+69.54	35.00 RT	305,745.52	634,882.96	0.00	SAWCUT; MATCH EXISTING
982	104+69.54	28.00 RT	305,751.32	634,886.89	1357.06	BOS
983	104+68.05	28.00 RT	305,752.16	634,885.65	1357.27	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
984	103+66.55	28.04 RT	305,809.06	634,801.61	1360.61	BOS
985	103+70.21	34.58 RT	305,801.59	634,800.97	0.00	SAWCUT; MATCH EXISTING
986	104+61.14	28.00 RT	305,756.03	634,879.94	1357.56	BOS; END CONCRETE DRIVEWAY 6-INCH
987	104+61.40	34.42 RT	305,750.57	634,876.56	0.00	SAWCUT; MATCH EXISTING
988	104+39.21	34.39 RT	305,763.04	634,858.20	0.00	SAWCUT; MATCH EXISTING
989	104+39.14	28.00 RT	305,768.37	634,861.72	1358.30	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
990	103+66.89	34.58 RT	305,803.45	634,798.22	0.00	SAWCUT; MATCH EXISTING
991	103+32.36	30.29 RT	305,826.37	634,772.04	0.00	SAWCUT; MATCH EXISTING
992	103+34.19	28.00 RT	305,827.24	634,774.84	1361.08	BOS; END CONCRETE DRIVEWAY 6-INCH
993	103+22.19	28.00 RT	305,833.98	634,764.91	1361.22	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
994	103+23.97	30.58 RT	305,830.85	634,764.93	0.00	SAWCUT; MATCH EXISTING
995	102+65.24	30.99 RT	305,863.45	634,716.08	0.00	SAWCUT; MATCH EXISTING
996	103+11.49	28.00 RT	305,839.98	634,756.05	1361.39	BOS; END CONCRETE DRIVEWAY 6-INCH
997	103+14.92	28.00 RT	305,838.05	634,758.89	1361.45	BOS; END ASPHALT BEHIND SIDEWALK
998	103+14.92	31.00 RT	305,835.57	634,757.21	0.00	SAWCUT; MATCH EXISTING
999	102+69.28	31.00 RT	305,861.18	634,719.42	0.00	SAWCUT; MATCH EXISTING
1000	102+69.45	28.00 RT	305,863.56	634,721.24	1362.49	BOS; BEGIN ASPHALT BEHIND SIDEWALK
1001	102+76.49	28.00 RT	305,859.61	634,727.07	1361.93	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
1002	102+65.26	28.00 RT	305,865.91	634,717.78	1362.57	BOS
1003	102+07.82	30.00 RT	305,896.48	634,669.11	0.00	SAWCUT; MATCH EXISTING
1004	101+96.33	28.00 RT	305,904.58	634,660.71	1364.13	BOS; END CONCRETE DRIVEWAY 6-INCH
1005	101+97.84	28.00 RT	305,903.73	634,661.96	1364.24	BOS; END ASPHALT BEHIND SIDEWALK
1006	101+97.84	30.00 RT	305,902.08	634,660.84	0.00	SAWCUT; MATCH EXISTING
1007	101+50.90	30.00 RT	305,928.41	634,621.98	0.00	SAWCUT; MATCH EXISTING
1008	101+50.90	28.00 RT	305,930.06	634,623.11	1365.89	BOS; BEGIN ASPHALT BEHIND SIDEWALK
1009	101+68.33	28.00 RT	305,920.29	634,637.53	1365.08	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
1010	102+07.82	28.00 RT	305,898.13	634,670.23	1363.93	BOS
1011	106+83.17	33.57 LT	305,699.88	635,097.47	0.00	SAWCUT; MATCH EXISTING
1012	106+87.41	33.68 LT	305,698.77	635,101.20	0.00	SAWCUT; MATCH EXISTING
1013	103+84.76	34.00 LT	305,850.20	634,851.48	0.00	SAWCUT; MATCH EXISTING
1014	104+03.70	34.00 LT	305,839.58	634,867.16	0.00	SAWCUT; MATCH EXISTING
1015	105+00.78	36.57 RT	305,726.71	634,907.94	0.00	SAWCUT; MATCH EXISTING
1016	105+03.42	36.57 RT	305,725.22	634,910.13	0.00	SAWCUT; MATCH EXISTING
1017	105+22.52	28.00 RT	305,721.60	634,930.75	1355.55	BOS
1018	105+22.52	36.55 RT	305,714.52	634,925.95	0.00	SAWCUT; MATCH EXISTING
1019	105+24.81	28.00 RT	305,720.32	634,932.64	1355.47	BOS
1020	105+24.81	36.55 RT	305,713.24	634,927.85	0.00	SAWCUT; MATCH EXISTING
1021	106+16.71	22.18 RT	305,674.46	635,015.45	1351.65	CURB GRADE BREAK



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					STATION	& OFFSET TA	BLE							
	POINT	STATION	0FFSET	Y COORDS	X COORDS	ELEVATION	REMARKS	POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
2	1100	107+39.19	32.00 LT	305,685.43	635,146.92	1346.45	BOS	1150	112+03.10	30.36 LT	305,629.14	635,606.41	1330.16	EOP; BEGIN RADIUS
	1101	107+58.67	32.00 LT	305,682.49	635,164.87	1345.51	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	1151	112+09.10	23.69 LT	305,621.50	635,611.52	0.00	BOC; CURB RAMP OPENING
	1102	107+59.64	38.17 LT	305,688.46	635,166.71	0.00	SAWCUT; MATCH EXISTING	1152	112+14.08	20.88 LT	305,617.81	635,616.13	0.00	BOC; CURB RAMP OPENING
_	1103	107+76.57	37.41 LT	305,685.26	635,183.35	0.00	SAWCUT; MATCH EXISTING	1153	112+17.00	18.00 LT	305,614.41	635,618.57	1329.47	EOP; END RADIUS
	1104	107+75.67	32.00 LT	305,680.04	635,181.69	1344.75	BOS; END CONCRETE DRIVEWAY 6-INCH	1154	112+17.00	33.00 LT	305,629.14	635,621.41	0.00	R =15'
	1105	107+41.93	32.00 LT	305,684.97	635,149.40	1346.34	BOS	1155	111+91.80	22.00 LT	305,622.67	635,593.41	1330.04	PROFILE GRADE BREAK
	1106	107+39.19	40.00 LT	305,693.29	635,148.40	0.00	SAWCUT; MATCH EXISTING	1156	112+17.79	67.79 RT	305,530.04	635,603.02	0.00	R = 105'
	1107	108+19.38	32.00 LT	305,673.72	635,224.94	1342.96	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	1157	112+46.82	33.26 LT	305,623.44	635,650.99	1329.56	BOS; END RADIUS; BEGIN RADIUS
	1108	108+20.56	39.59 LT	305,681.06	635,227.20	0.00	SAWCUT; MATCH EXISTING	1158	112+73.96	29.50 LT	305,614.38	635,676.86	1328.85	BOS; END RADIUS
	1109	108+37.48	38.08 LT	305,677.12	635,243.73	0.00	SAWCUT; MATCH EXISTING	1159	112+73.96	129.50 LT	305,712.39	635,696.68	0.00	R = 100'
	1110	108+37.06	32.00 LT	305,671.16	635,242.43	1342.48	BOS; END CONCRETE DRIVEWAY 6-INCH	1160	112+77.14	18.00 RT	305,575.00	635,616.26	1329.26	CURB GRADE BREAK
	1111	107+41.93	40.00 LT	305,692.84	635,150.83	0.00	SAWCUT; MATCH EXISTING	1161	112+68.54	15.00 RT	305,571.84	635,662.72	1328.23	EDGE OF CONCRETE
	1112	109+34.81	32.00 LT	305,657.04	635,339.16	1337.94	BOS	1162	112+68.54	18.00 RT	305,568.90	635,662.12	1328.14	EOP; END RADIUS; END CONCRETE CURB & GUTTER
	1113	109+65.99	31.92 LT	305,653.20	635,368.56	1337.31	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	1163	112+68.54	20.50 RT	305,566.45	635,661.63	0.00	EDGE OF ASPHALT SHOULDER
	1114	109+66.33	31.92 LT	305,653.17	635,368.88	1337.25	BOS	1164	112+68.54	33.00 RT	305,554.19	635,659.15	0.00	R = 15'
	1115	109+66.94	40.08 LT	305,661.23	635,370.22	0.00	SAWCUT; MATCH EXISTING	1165	112+56.32	35.62 RT	305,554.05	635,646.65	1329.20	EDGE OF ASPHALT SHOULDER
Ī	1116	109+77.88	39.46 LT	305,659.67	635,380.76	0.00	SAWCUT; MATCH EXISTING	1166	112+53.87	36.14 RT	305,554.02	635,644.15	1328.83	EOP; BEGIN RADIUS; BEGIN ASPHALTIC SURFACE; END CONCRETE CURB & GUTTER
Ī	1117	109+77.29	31.90 LT	305,652.19	635,379.51	1336.67	BOS; END CONCRETE DRIVEWAY 6-INCH	1167	112+55.50	48.11 RT	305,541.97	635,643.37	0.00	EOP; SAWCUT; MATCH EXISTING
	1118	109+79.52	31.90 LT	305,652.00	635,381.72	1336.59	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	1168	112+42.14	38.65 RT	305,553.89	635,632.15	1329.14	PROFILE GRADE BREAK
ı	1119	109+80.09	39.33 LT	305,659.35	635,382.95	0.00	SAWCUT; MATCH EXISTING	1169	112+44.72	50.69 RT	305,541.58	635,632.29	0.00	BEGIN PROFILE GRADE; MATCH EXISTING
ı	1120	110+01.16	38.07 LT	305,656.25	635,403.83	0.00	SAWCUT; MATCH EXISTING	1170	112+31.73	53.80 RT	305,541.11	635,618.94	0.00	EOP; SAWCUT; MATCH EXISTING
ı	1121	110+00.62	31.90 LT	305,650.15	635,402.74	1335.90	BOS; END CONCRETE DRIVEWAY 6-INCH	1171	112+28.53	41.69 RT	305,553.61	635,618.21	1329.33	EOP
ı	1122	109+37.12	32.00 LT	305,656.70	635,341.44	1337.85	BOS	1172	112+30.95	41.05 RT	305,553.76	635,620.71	1329.29	EOP; BEGIN ASPHALTIC SURFACE; END CONCRETE CURB & GUTTER
F	1123	109+34.81	42.00 LT	305,666.93	635,340.60	0.00	SAWCUT; MATCH EXISTING	1173	112+37.72	18.00 RT	305,575.01	635,631.91	1328.82	PROFILE GRADE BREAK
ŀ	1124	110+65.08	31.90 LT	305,644.49	635,466.96	1333.82	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH	1174	112+27.84	29.25 RT	305,565.94	635,620.00	1329.21	EOP; END RADIUS
F	1125	110+65.08	34.89 LT	305,647.47	635,467.22	0.00	SAWCUT; MATCH EXISTING	1175	112+12.75	33.00 RT	305,565.07	635,605.02	0.00	R = 15'
ŀ	1126	111+00.07	35.91 LT	305,645.42	635,502.16	0.00	SAWCUT; MATCH EXISTING	1176	112+25.12	28.91 RT	305,566.82	635,617.40	0.00	BOC; CURB RAMP OPENING
l	1127	111+00.07	31.90 LT	305,641.43	635,501.81	1332.45	BOS; END CONCRETE DRIVEWAY 6-INCH	1177	112+17.36	28.89 RT	305,619.59	635,609.84	1329.63	CURB GRADE BREAK
ŀ	1128	109+37.12	42.00 LT	305,666.60	635,342.88	0.00	SAWCUT; MATCH EXISTING	1178	111+69.76	42.77 LT	305,646.08	635,572.61	0.00	SAWCUT; MATCH EXISTING
ŀ	1129	111+63.46	22.00 LT	305,626.00	635,564.10	1330.76	EOP; BEGIN RADIUS	1179	112+21.83	23.91 RT	305,572.37	635,615.17	0.00	BOC; CURB RAMP OPENING
ı	1130	111+63.46	37.00 LT	305,640.95	635,565.41	0.00	R = 15'	1181	112+17.19	21.23 RT	305,575.86	635,611.33	0.00	BOC; CURB RAMP OPENING
ı	1131	111+65.89	26.90 LT	305,630.67	635,567.00	0.00	FRONT OF SIDEWALK; BEGIN RADIUS; SEE DETAIL	1182	112+11.91	20.50 RT	305,577.51	635,606.49	0.00	BOC; CURB RAMP OPENING
ı	1132	111+65.88	31.90 LT	305,635.65	635,567.44	0.00	R = 5'	1183	111+69.27	48.98 LT	305,652.31	635,572.68	0.00	SAWCUT; MATCH EXISTING
ı	1133	111+70.57	32.28 LT	305,635.55	635,572.44	0.00	FRONT OF SIDEWALK; END RADIUS; SEE DETAIL	1184	112+12.75	18.00 RT	305,579.82	635,607.74	1329.39	EOP; BEGIN RADIUS
ı	1134	111+74.32	32.10 LT	305,634.97	635,576.39	0.00	BOC; CURB RAMP OPENING	1185	112+03.49	28.61 RT	305,570.89	635,597.21	1329.96	BOS; END CONCRETE DRIVEWAY 6-INCH
- 1-				305,639.81		0.00	BOC; CURB RAMP OPENING	1186	108+00.08	38.00 LT	305,682.45	635,206.70	0.00	SAWCUT; MATCH EXISTING
⊢	_			305,681.91		0.00	SAWCUT; MATCH EXISTING	1187	108+00.08			635,205.83	1343.74	BOS
H				305,640.94	ļ	1330.88	EOP; END RADIUS; BEGIN ASPHALTIC SURFACE	1188	111+74.87			635,570.45		BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
H		111+65.15	42.38 LT	305,646.15	635,567.60	0.00	BOS; SAWCUT; MATCH EXISTING		111+70.90	28.60 RT	305,574.94	635,566.72	1331.00	BOS
-				305,652.31		0.00	EOP; SAWCUT; MATCH EXISTING	1190	110+08.54			635,405.09	1336.95	
- 1-				305,652.31		0.00	BEGIN PROFILE GRADE; MATCH EXISTING	1191	109+69.31			635,366.10	1338.16	
⊢	1141				635,606.41	0.00	EOP; SAWCUT; MATCH EXISTING	1192	109+27.48			635,323.18	1339.55	
⊢				305,640.94		1330.75	EOP; BEGIN ASPHALTIC SURFACE	1193	108+74.33			635,270.42	1341.66	
- 1-				305,652.31		0.00	BOS; SAWCUT; MATCH EXISTING	1194	108+11.92			635,208.60	1344.34	
⊢				305,635.29		0.00	BOC; CURB RAMP OPENING	1195	107+42.75			635,139.59	1347.48	
⊢				305,572.74		0.00	SAWCUT; MATCH EXISTING	1196	107+04.93			635,100.19	1349.00	
⊢				305,634.05		0.00	BOS; BEGIN RADIUS; SEE DETAIL	1197	107+08.67			635,104.05	1348.99	
- 1-				305,675.98		ļ	BOS					635,098.54		SAWCUT; MATCH EXISTING
-				305,568.98		0.00	SAWCUT: MATCH EXISTING				305,627.30	+		SAWCUT; MATCH EXISTING
⊢				305,630.24		0.00	BOC; CURB RAMP OPENING			5.101 KI	202,021100	,202,21	3.00	
ŀ	73		31.00 L1	1 303,030.24	300,000.31	J 3.00	200, COMB TISME OF ENTINO							

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HWY:STH 33

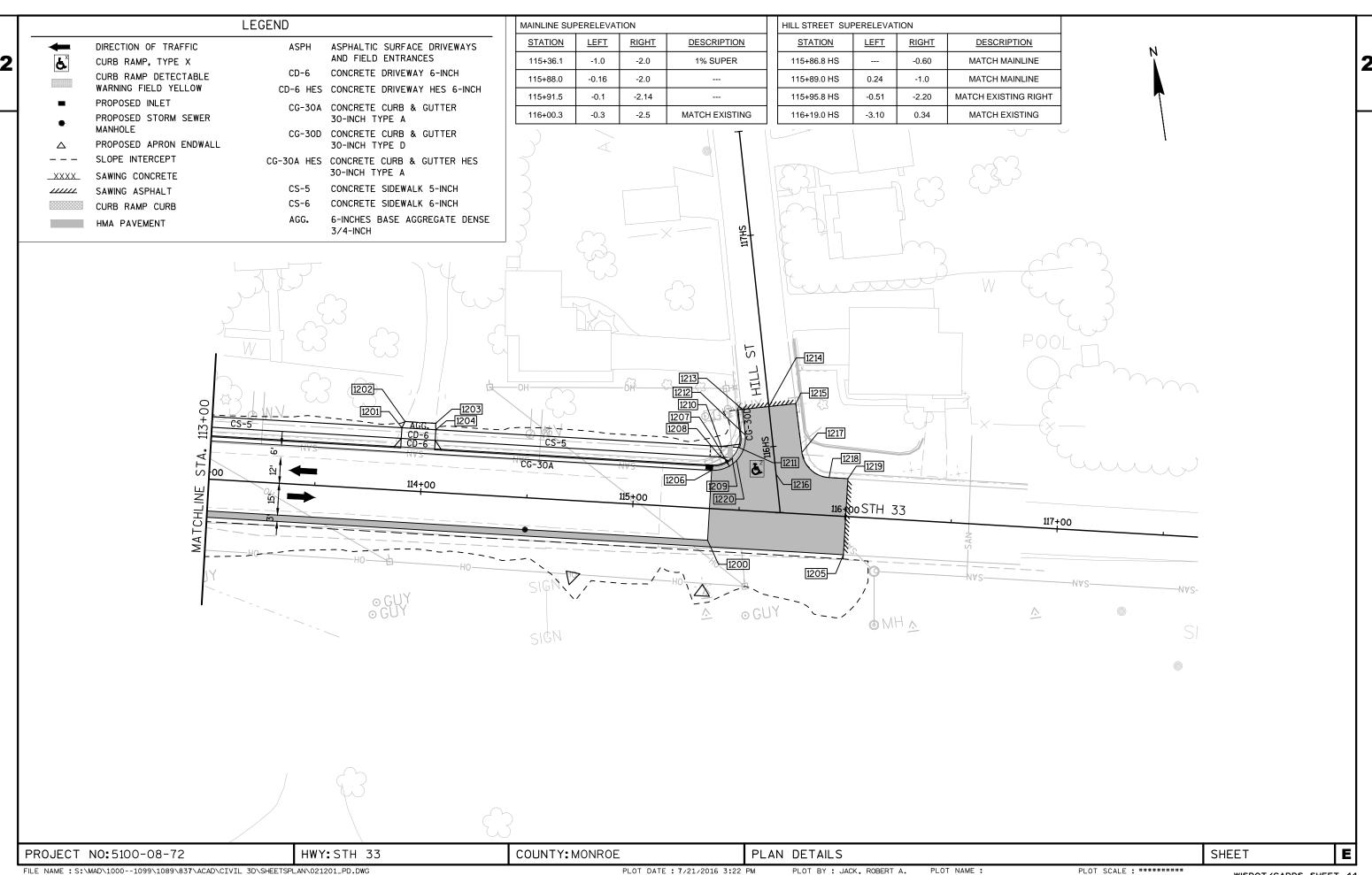
PROJECT NO:5100-08-72

COUNTY: MONROE

PLAN DETAILS

SHEET

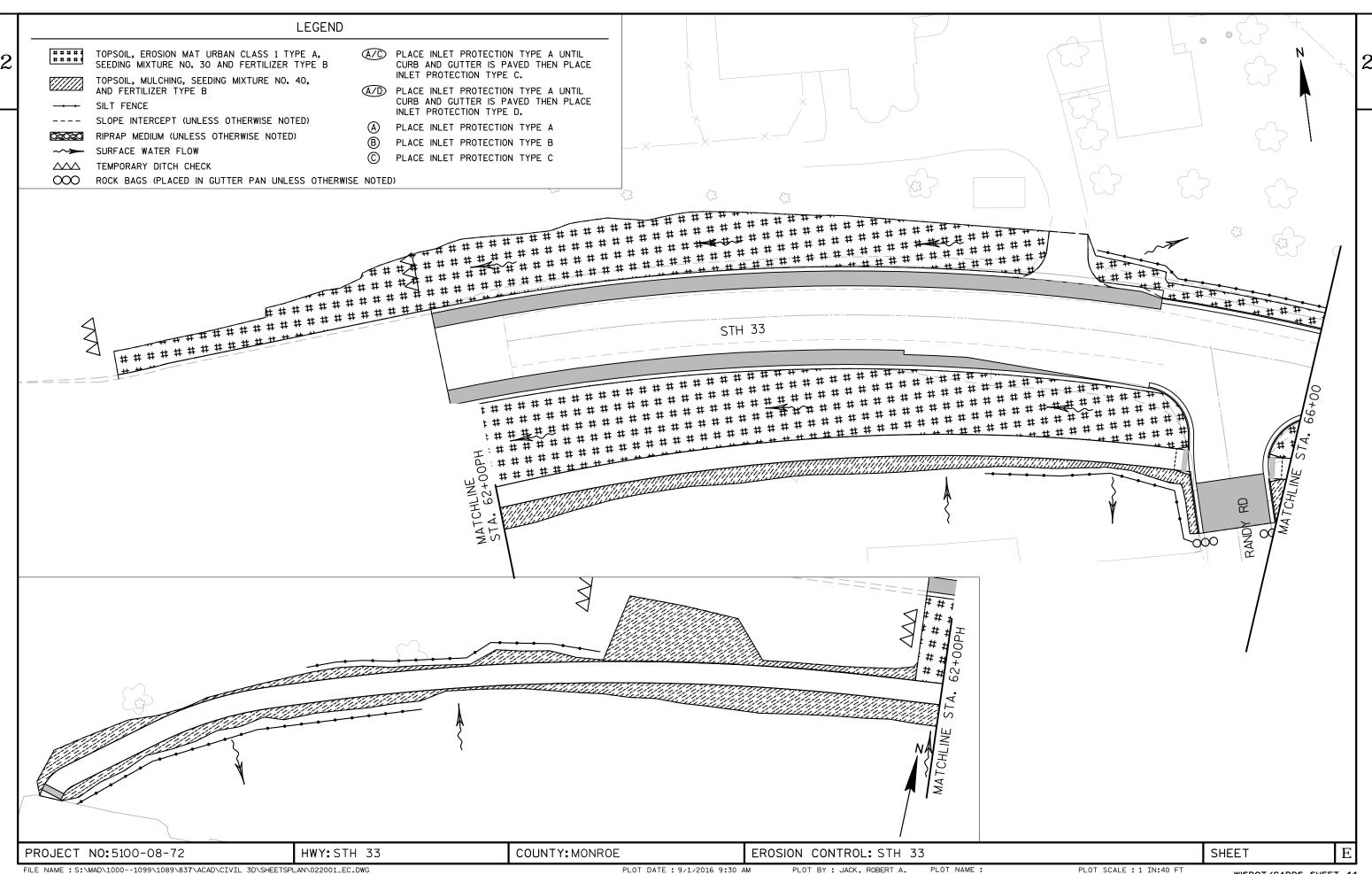
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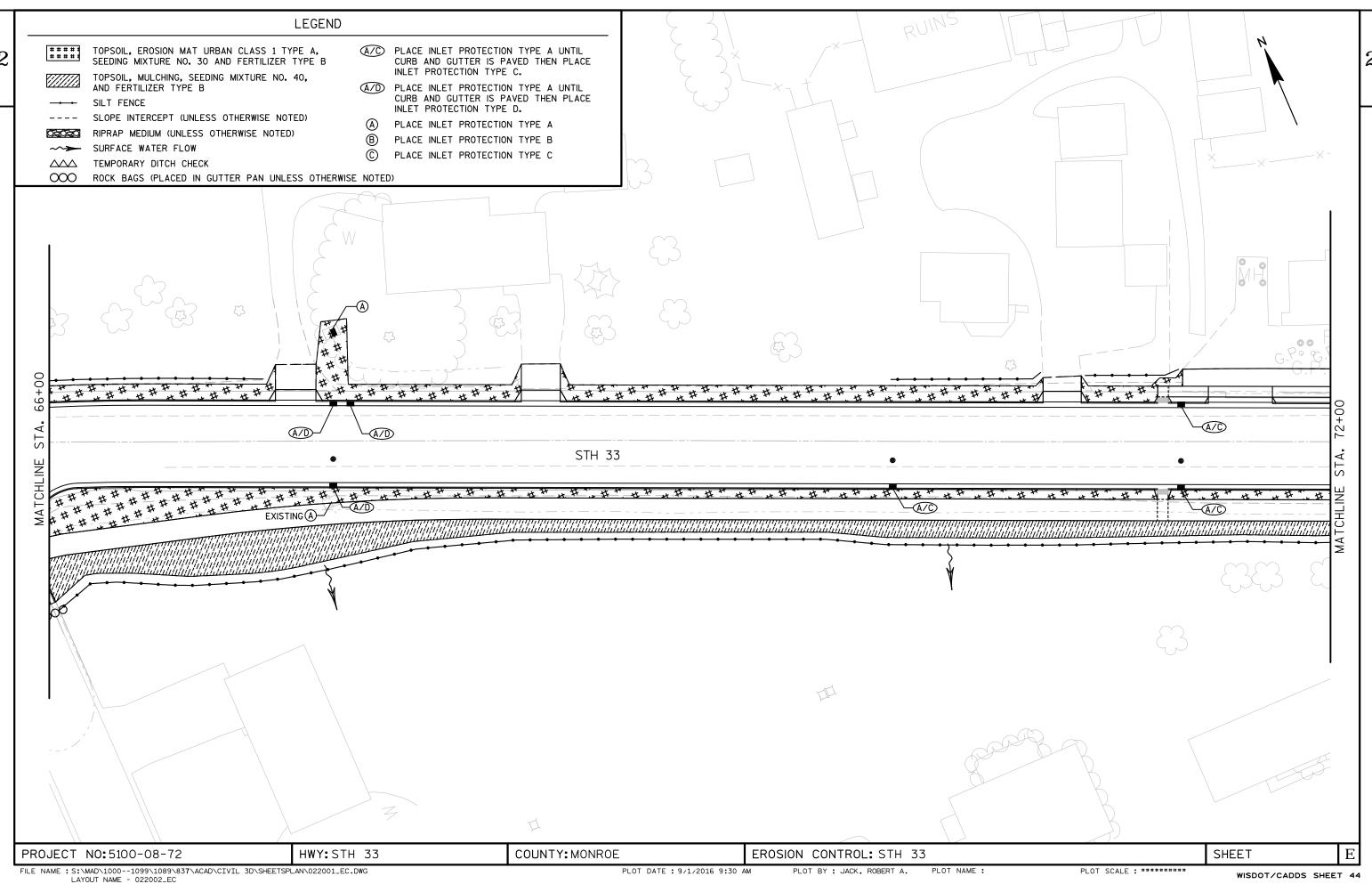


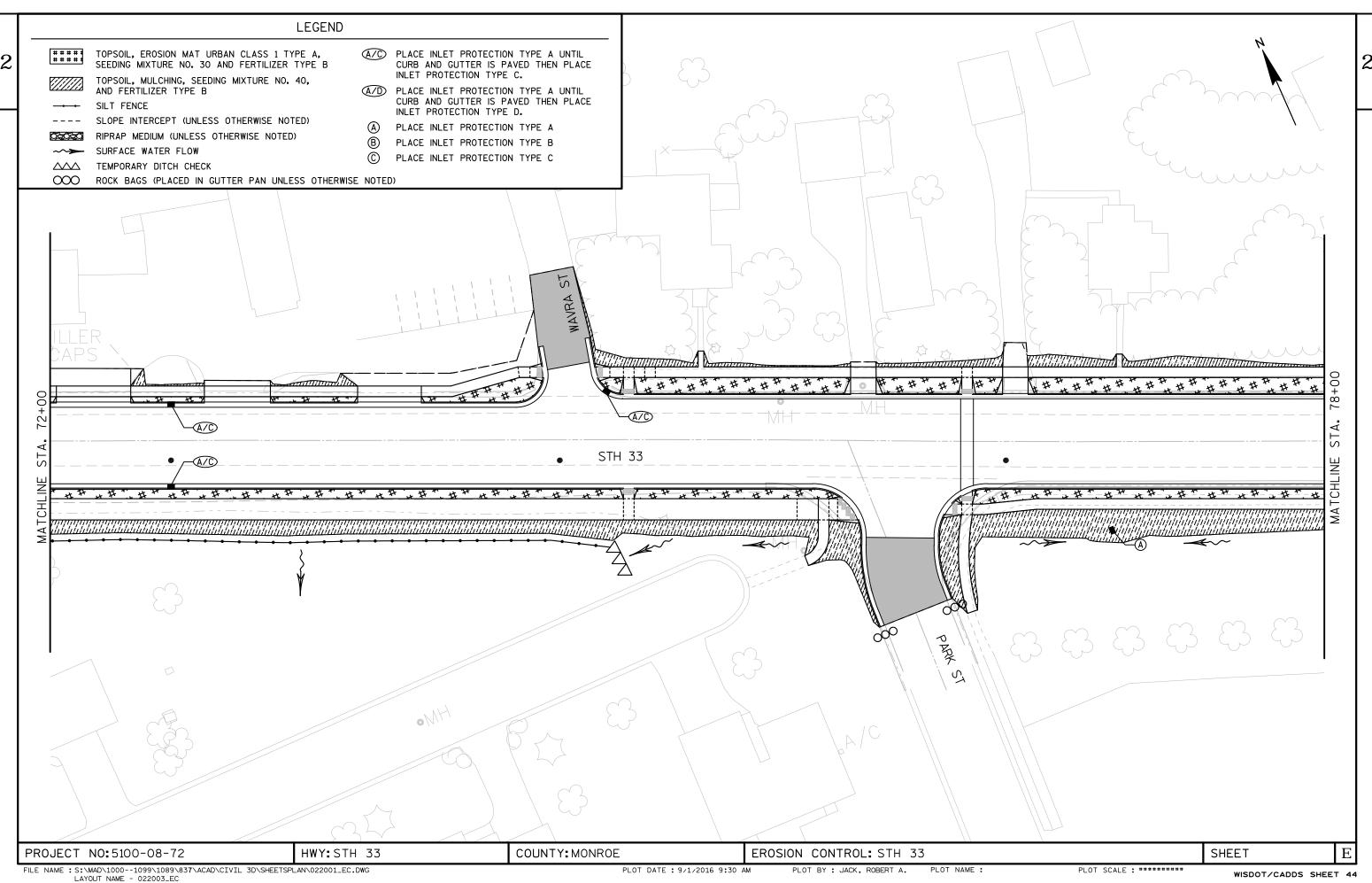
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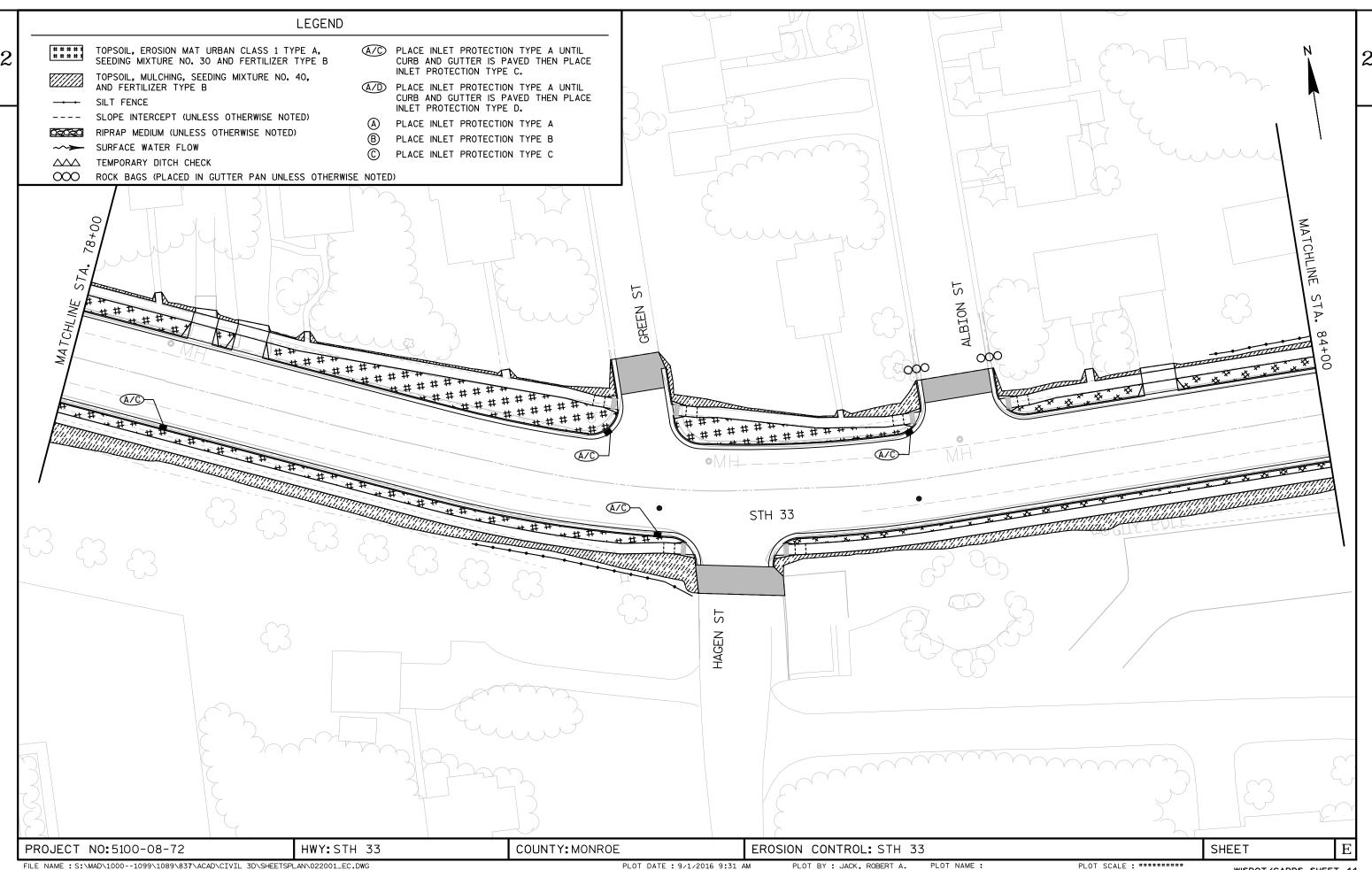
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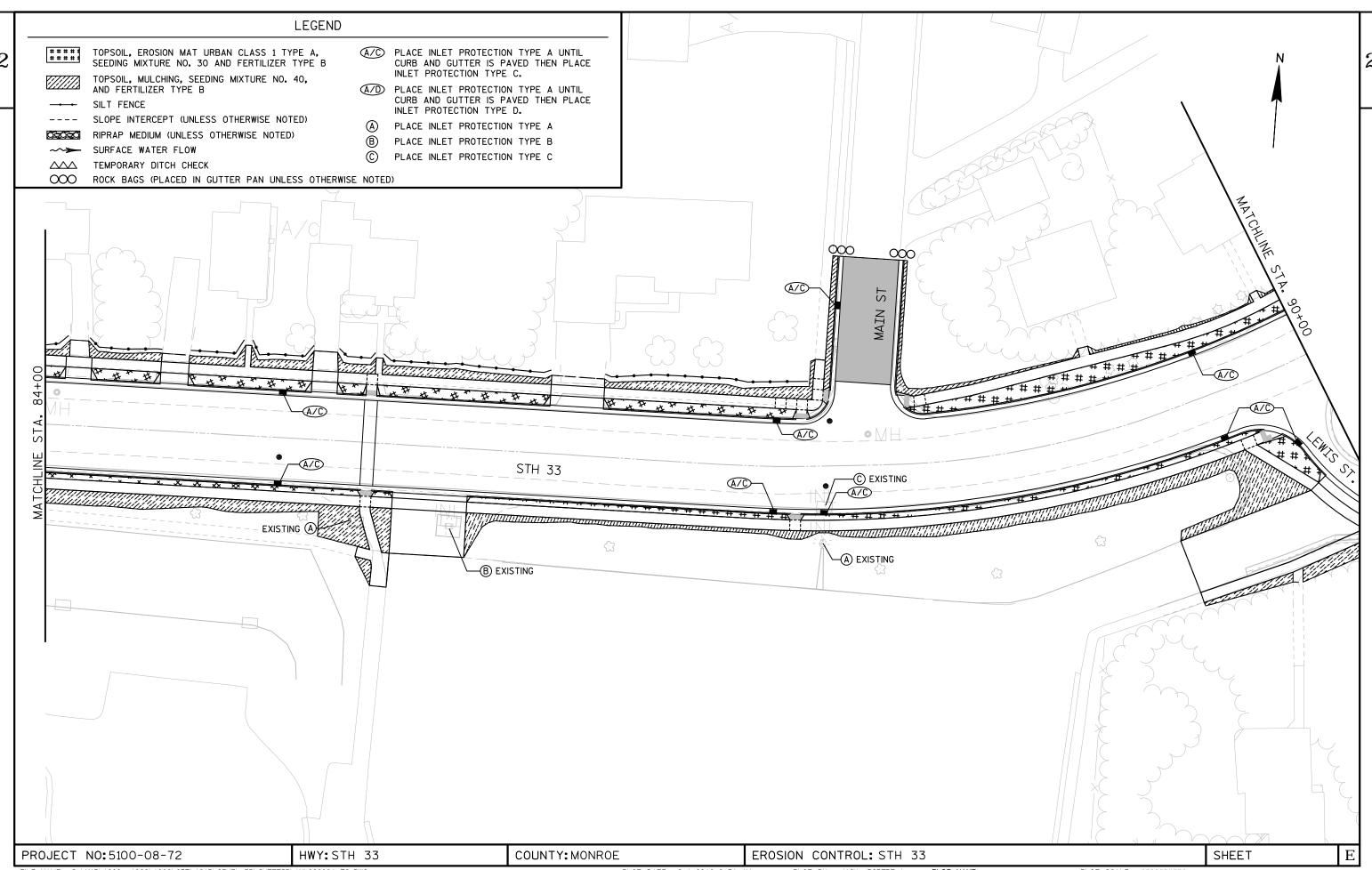
				STATION & OF	FSET TABLE	
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	REMARKS
1200	115+36.07	15.00 RT	305,518.79	635,924.94	1323.74	EDGE OF CONCRETE; BEGIN HMA PAVEMENT
1201	113+89.35	29.50 LT	305,591.50	635,789.95	1326.86	BOS; BEGIN CONCRETE DRIVEWAY 6-INCH
1202	113+90.66	32.50 LT	305,594.18	635,791.83	0.00	MATCH EXISTING
1203	114+05.07	32.50 LT	305,591.32	635,805.96	1326.57	MATCH EXISTING
1204	114+05.35	29.50 LT	305,588.33	635,805.63	1326.35	BOS; END CONCRETE DRIVEWAY 6-INCH
1205	116+00.31	18.03 RT	305,503.08	635,987.30	0.00	EOP; SAWCUT; MATCH EXISTING
1206	115+36.07	18.00 LT	305,551.13	635,931.48	1323.86	EOP; BEGIN RADIUS; BEGIN HMA PAVEMENT
1207	115+36.07	33.00 LT	305,565.84	635,934.46	0.00	R = 15'
1208	115+39.91	24.50 LT	305,556.74	635,936.53	0.00	FRONT OF SIDEWALK
1209	115+45.24	24.50 LT	305,555.69	635,941.75	0.00	BOC; CURB RAMP OPENING
1210	115+39.91	29.50 LT	305,561.64	635,937.52	0.00	BOS
1211	115+48.07	29.50 LT	305,560.03	635,945.52	0.00	BOC; CURB RAMP OPENING
1212	115+50.87	35.47 LT	305,565.32	635,949.45	1324.83	EOP; END RADIUS
1213	115+48.86	47.52 LT	305,577.53	635,949.86	0.00	EOP; SAWCUT; MATCH EXISTING
1214	115+61.19	49.58 LT	305,577.11	635,962.36	0.00	BEGIN PROFILE GRADE; MATCH EXISTING
1215	115+73.74	51.68 LT	305,576.67	635,975.08	0.00	EOP; SAWCUT; MATCH EXISTING
1216	115+66.48	17.91 LT	305,545.02	635,961.26	1323.37	PROFILE GRADE BREAK
1217	115+78.00	28.87 LT	305,553.48	635,974.73	0.00	EOP; MATCH EXISTING
1218	115+91.45	17.83 LT	305,539.99	635,985.73	0.00	EOP; MATCH EXISTING
1219	116+00.31	17.80 LT	305,538.20	635,994.40	0.00	EOP; SAWCUT; MATCH EXISTING
1220	115+47.52	23.31 LT	305,554.07	635,943.76	1323.99	CURB GRADE BREAK
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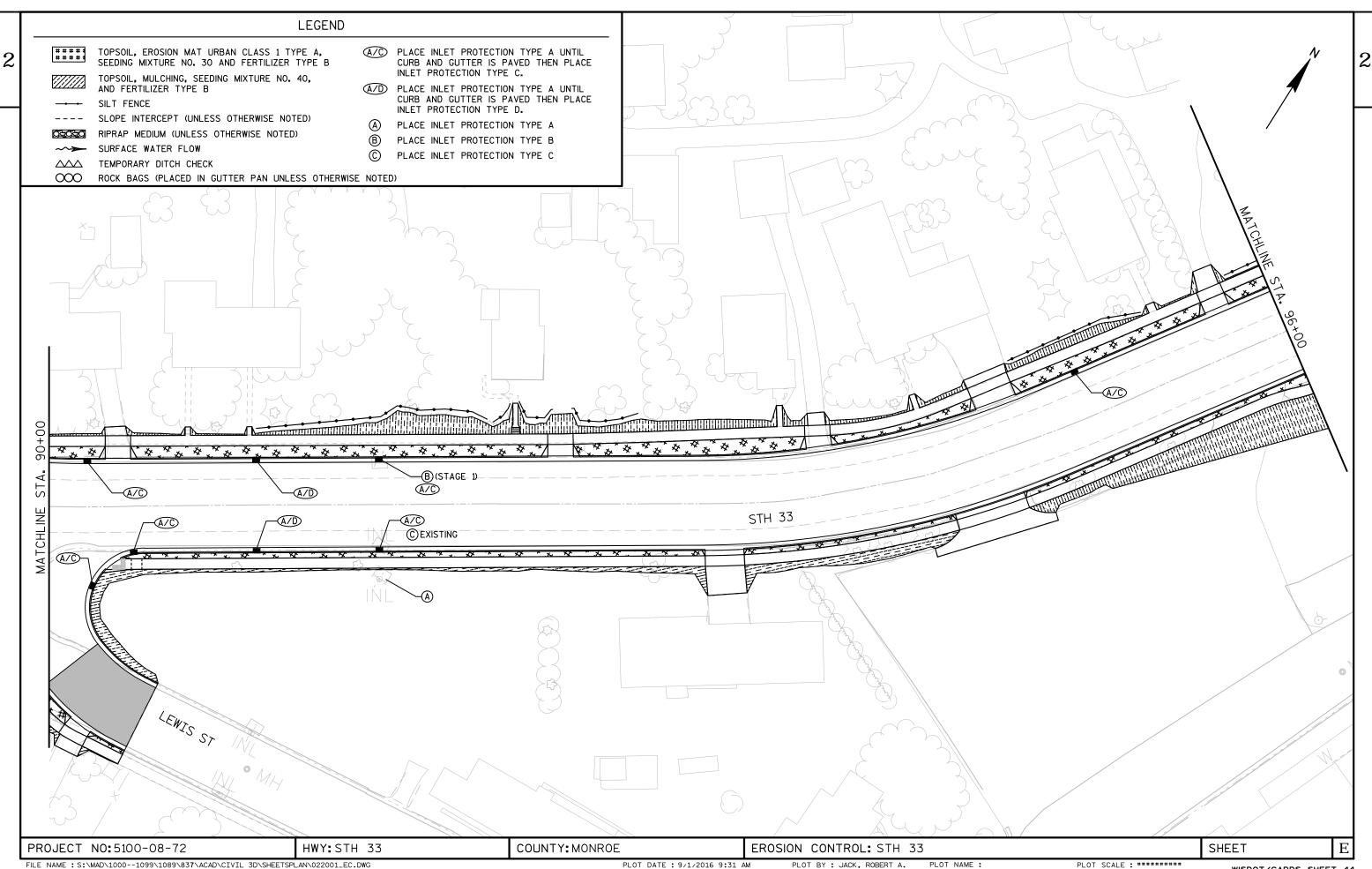


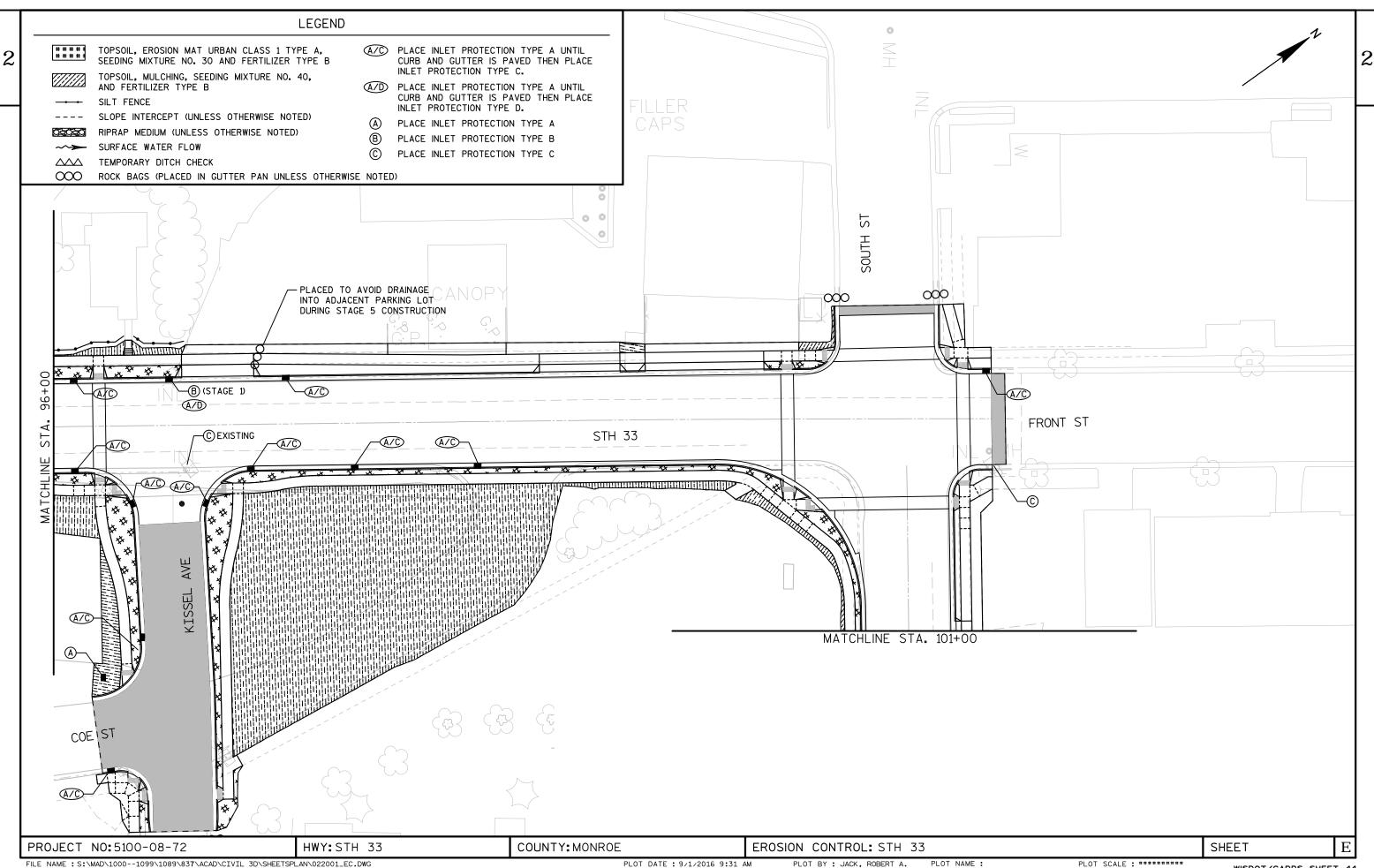


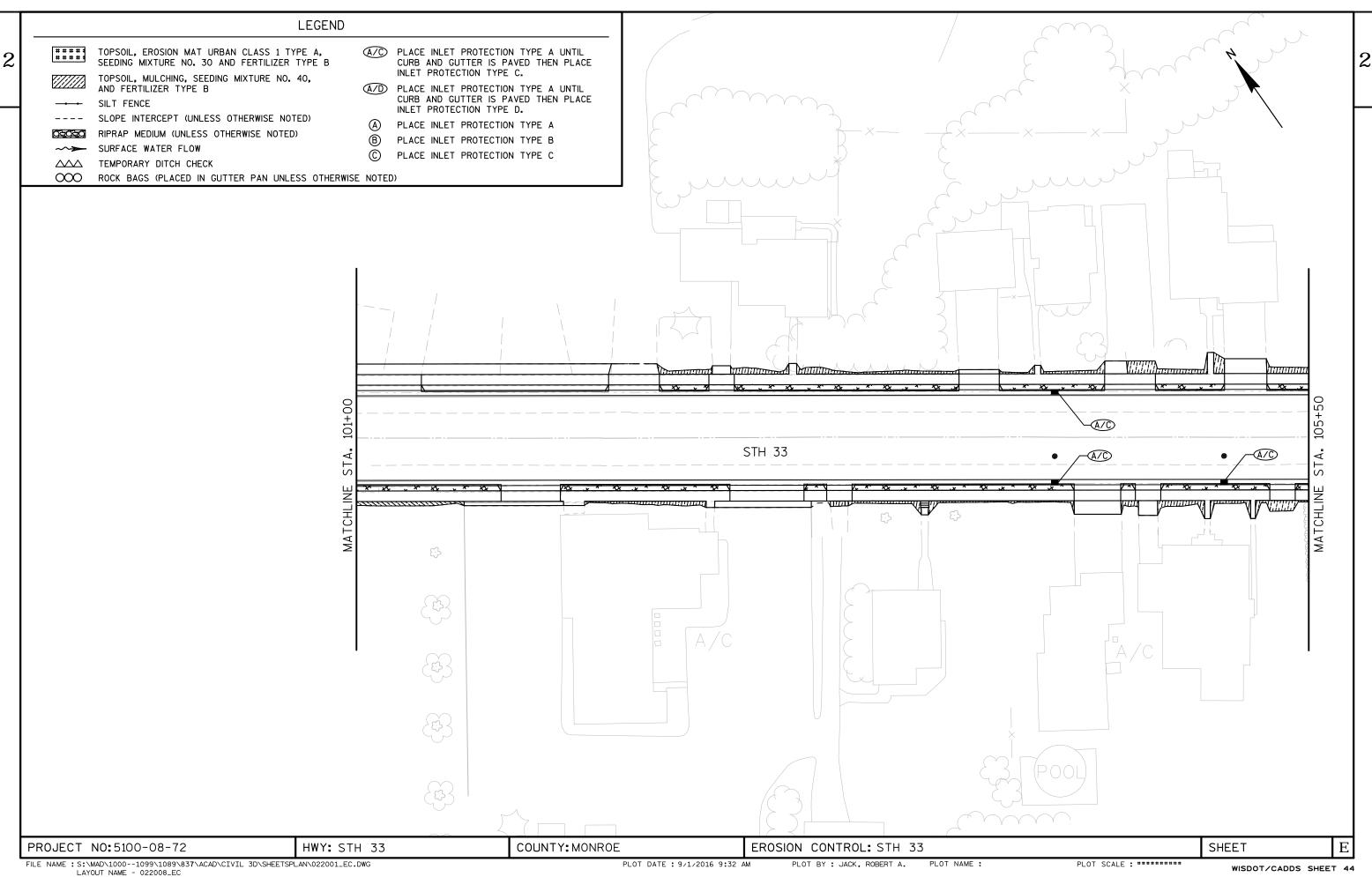


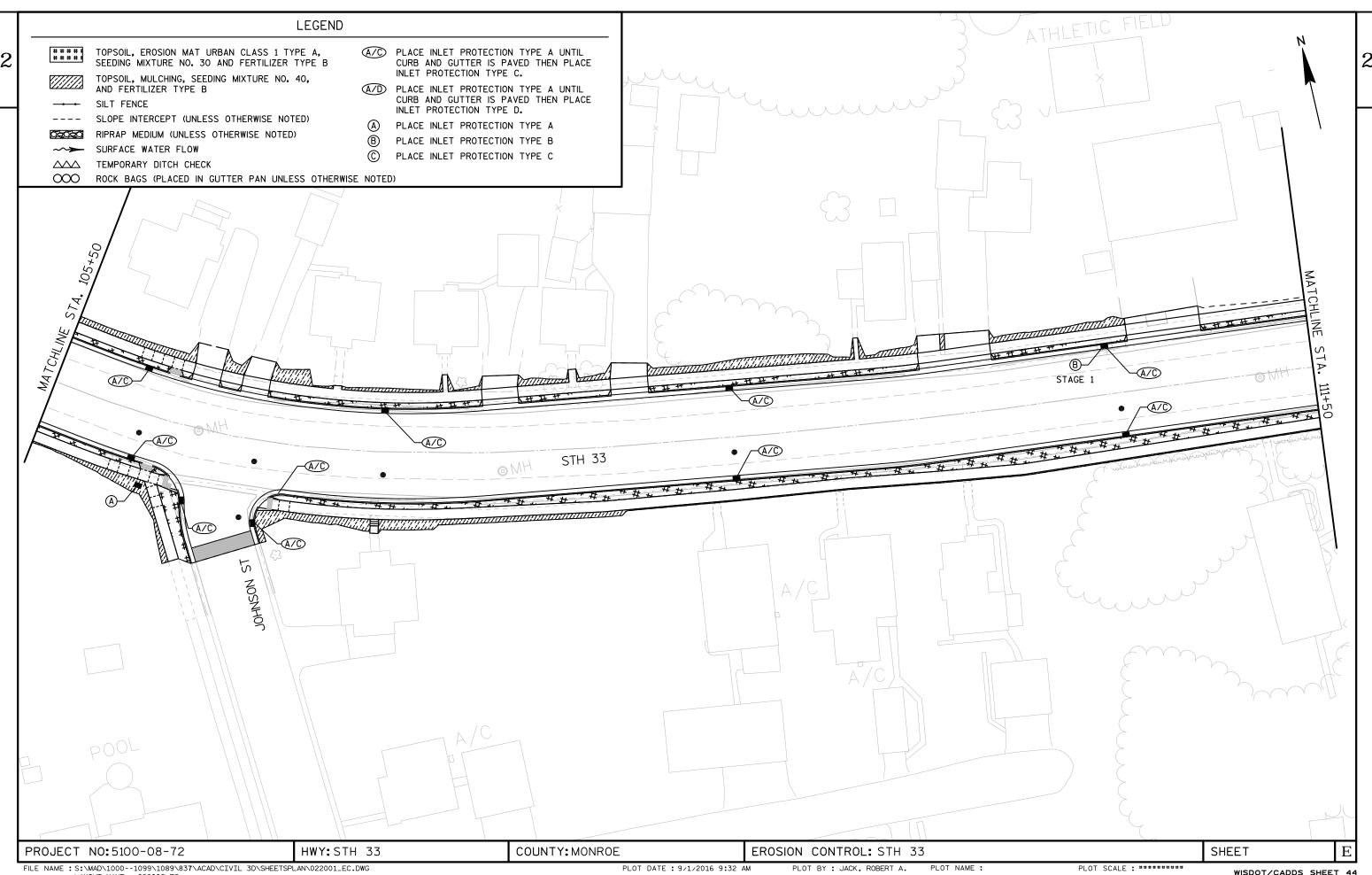


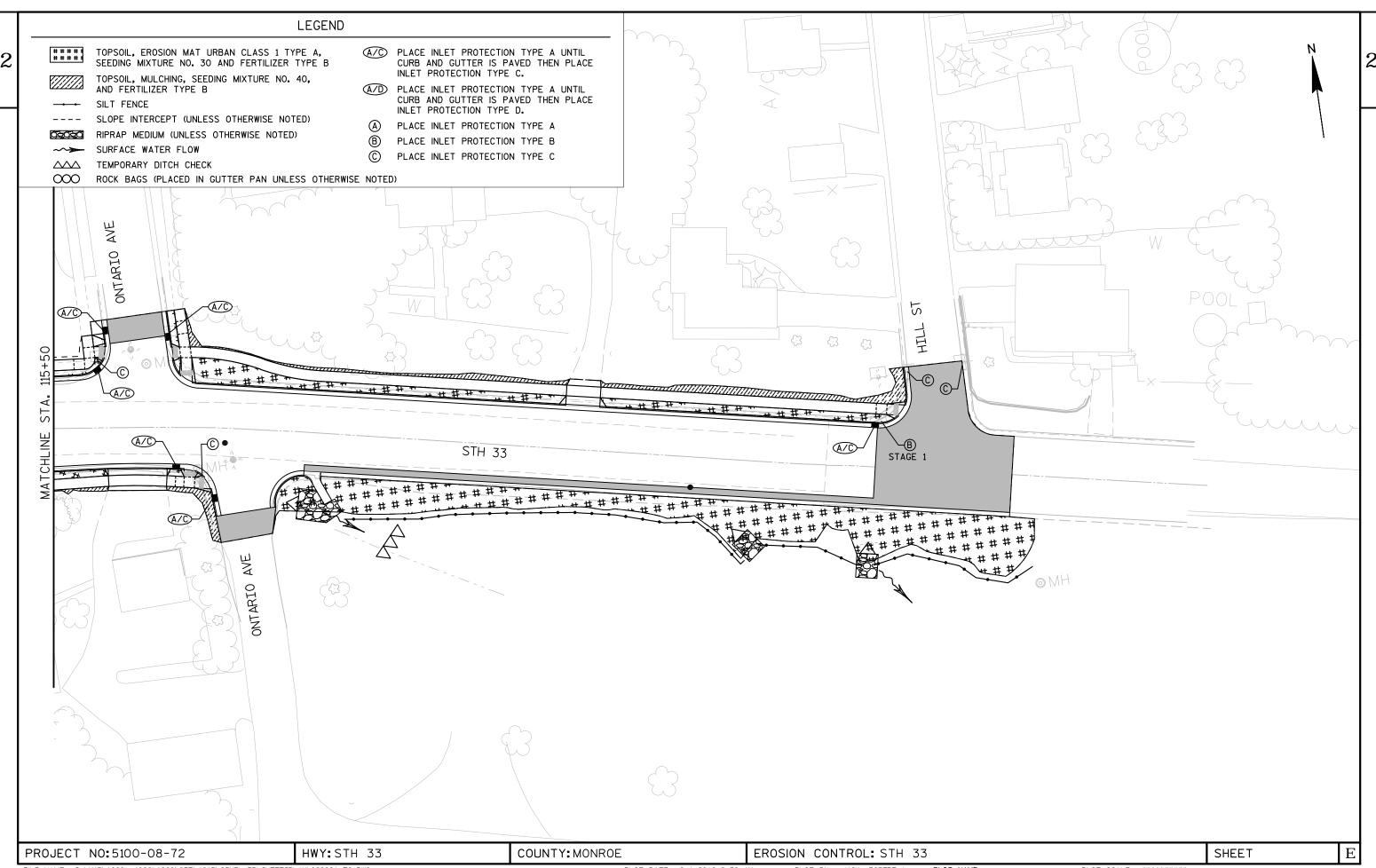


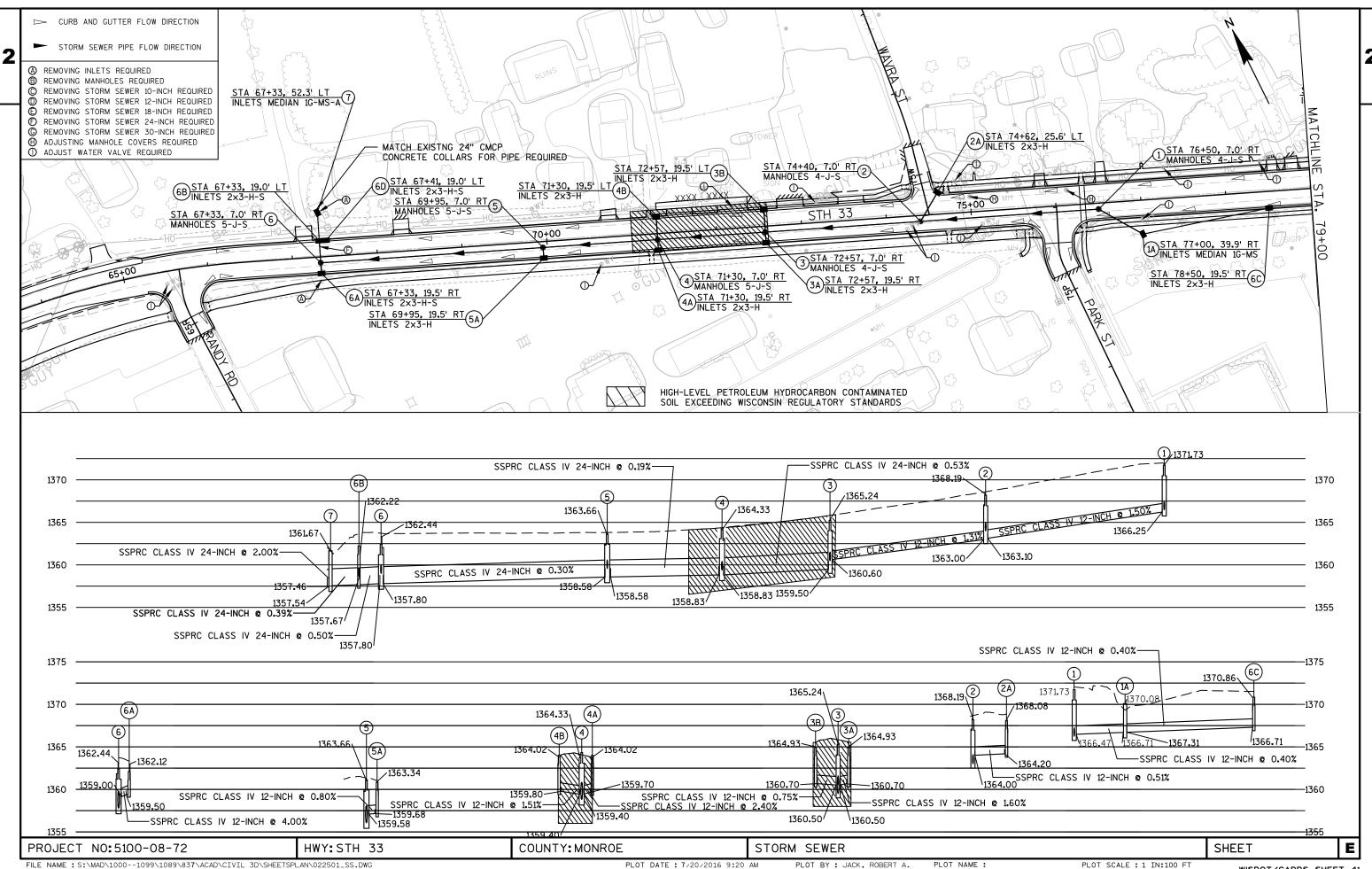


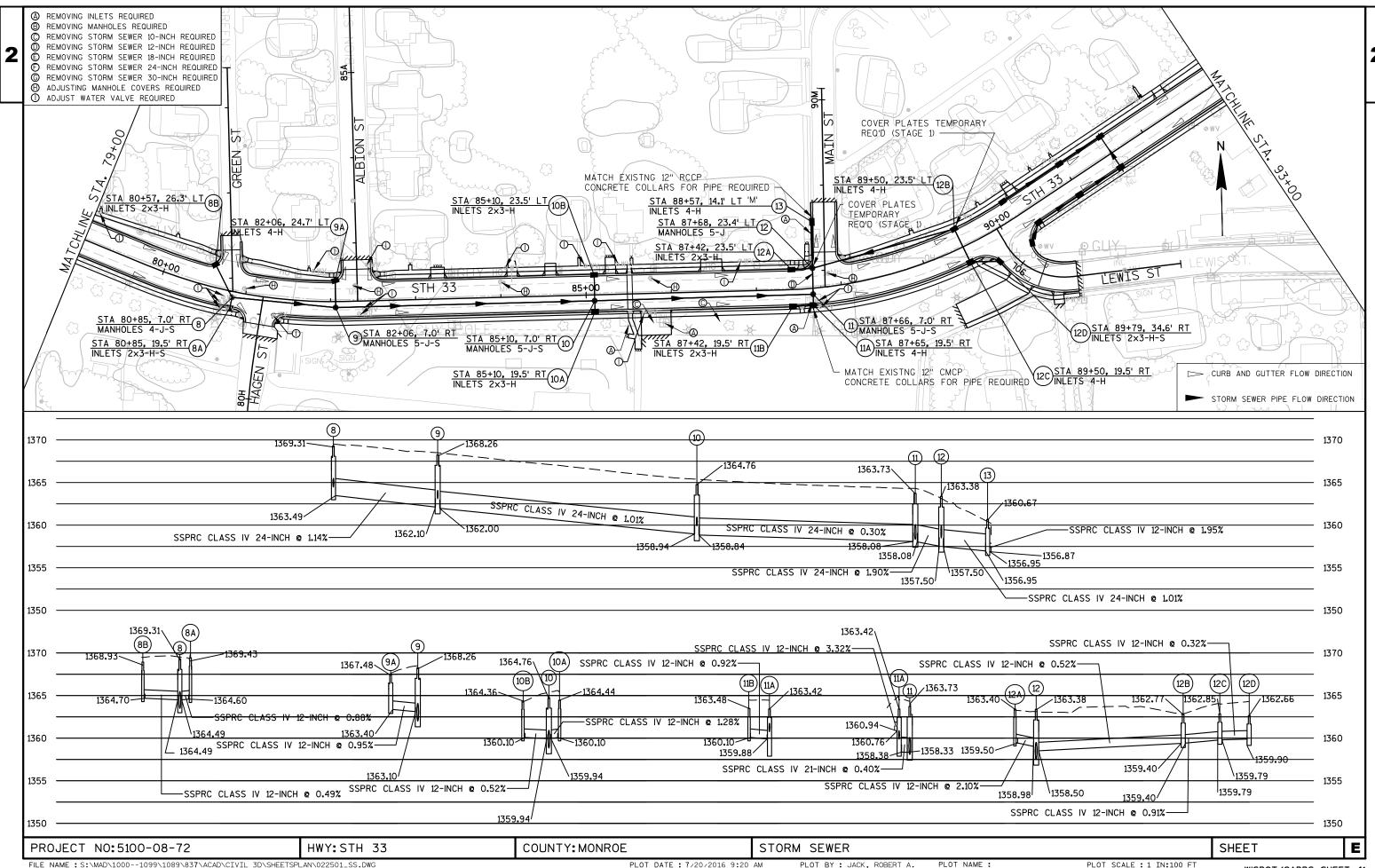


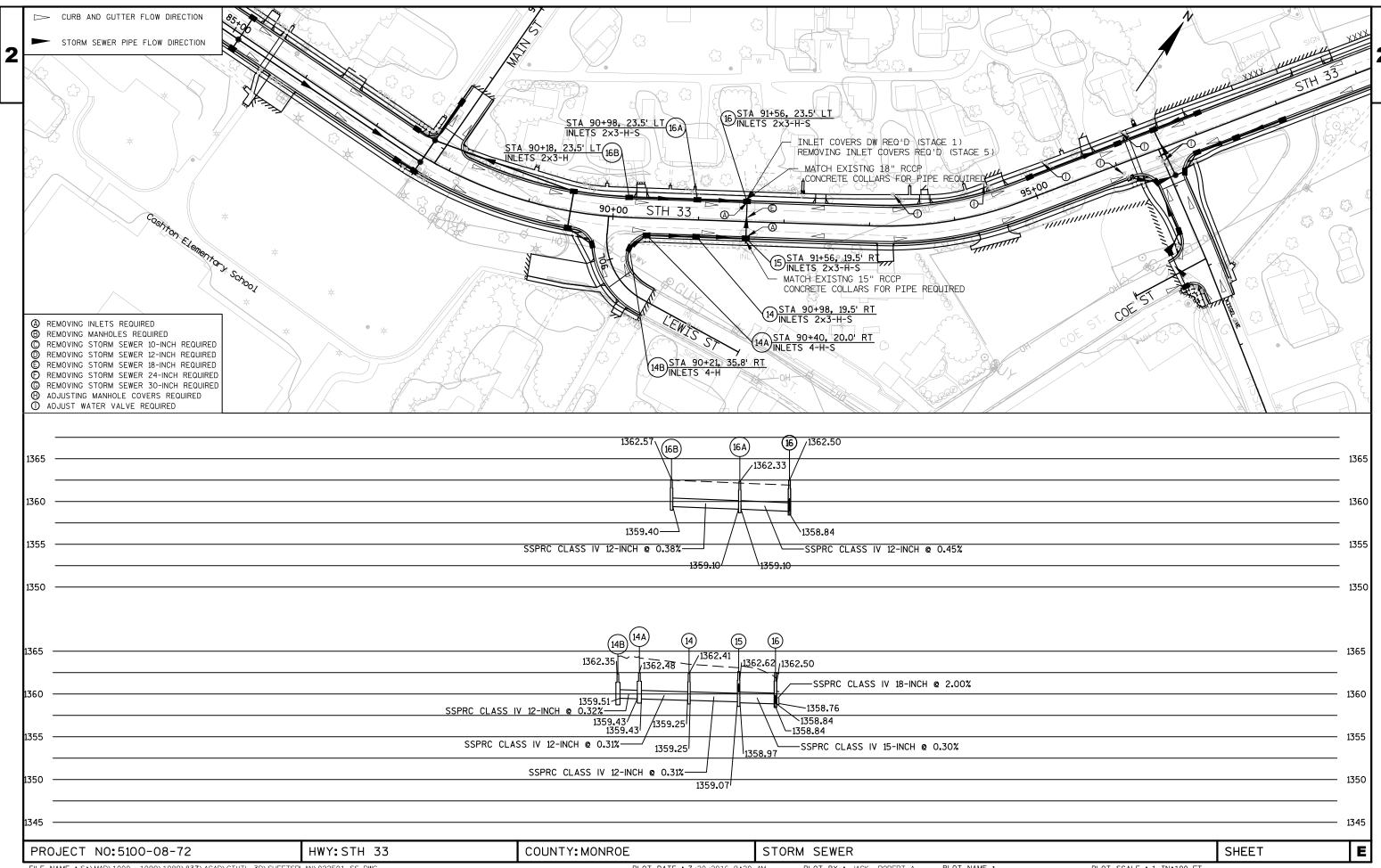


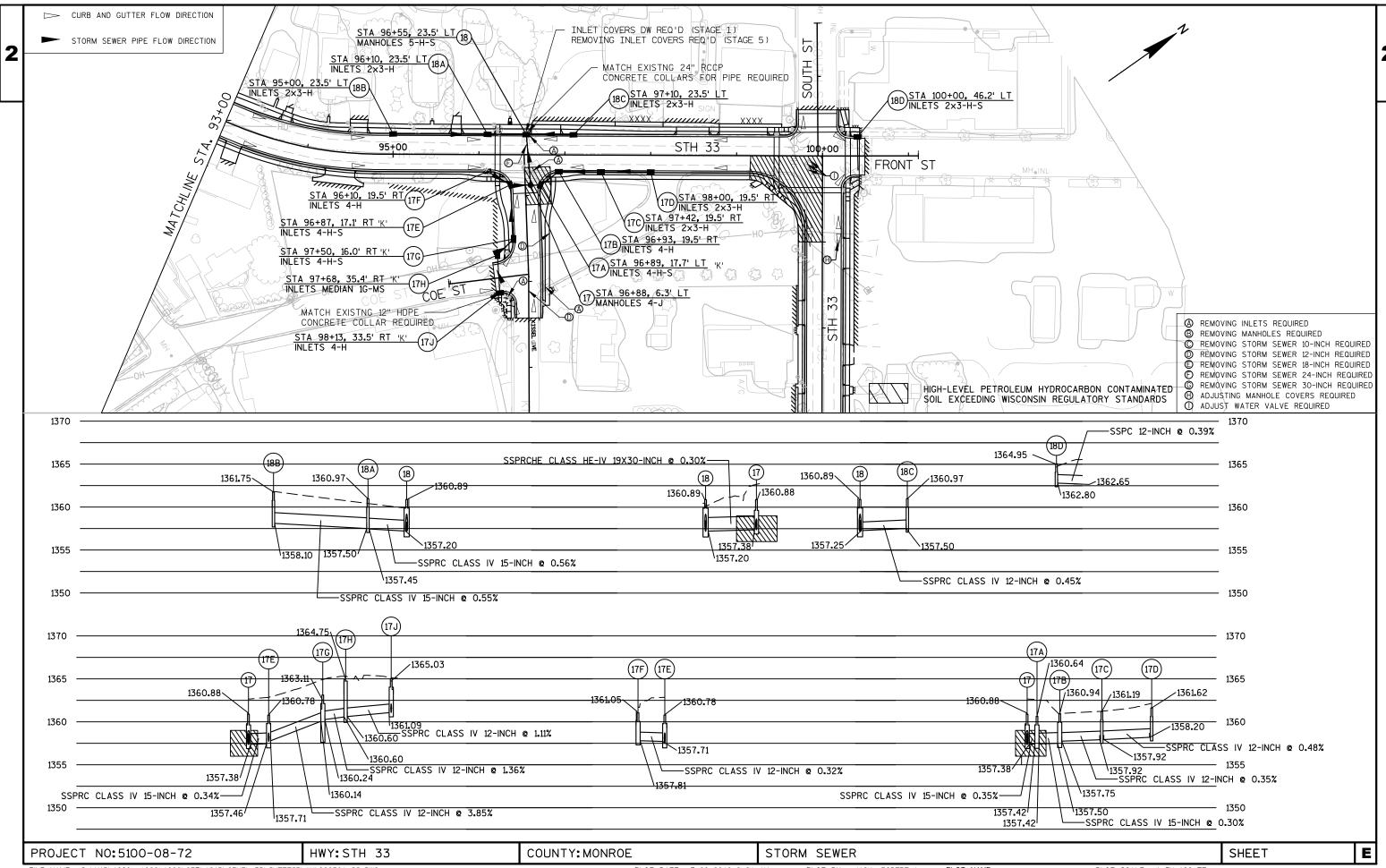


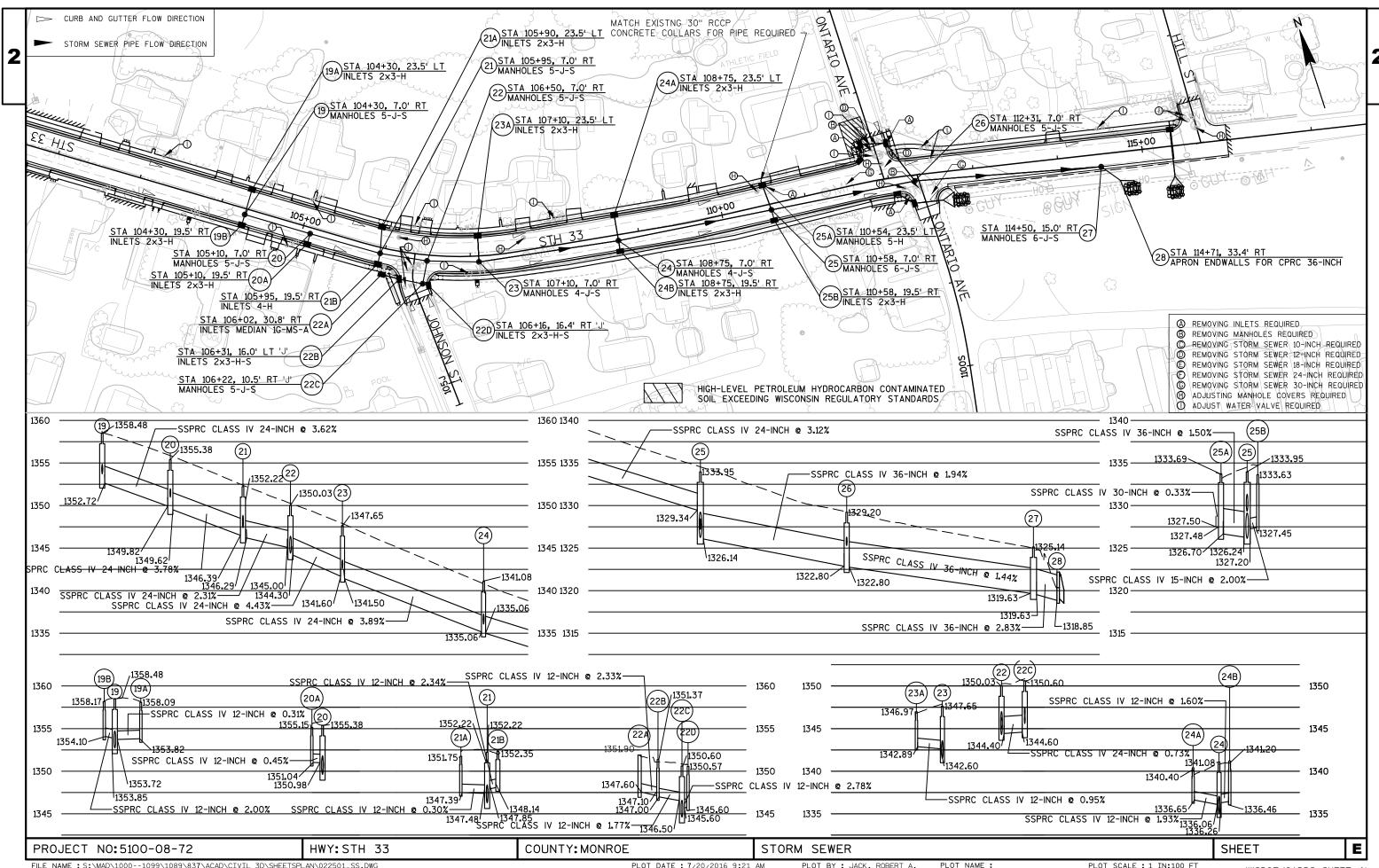


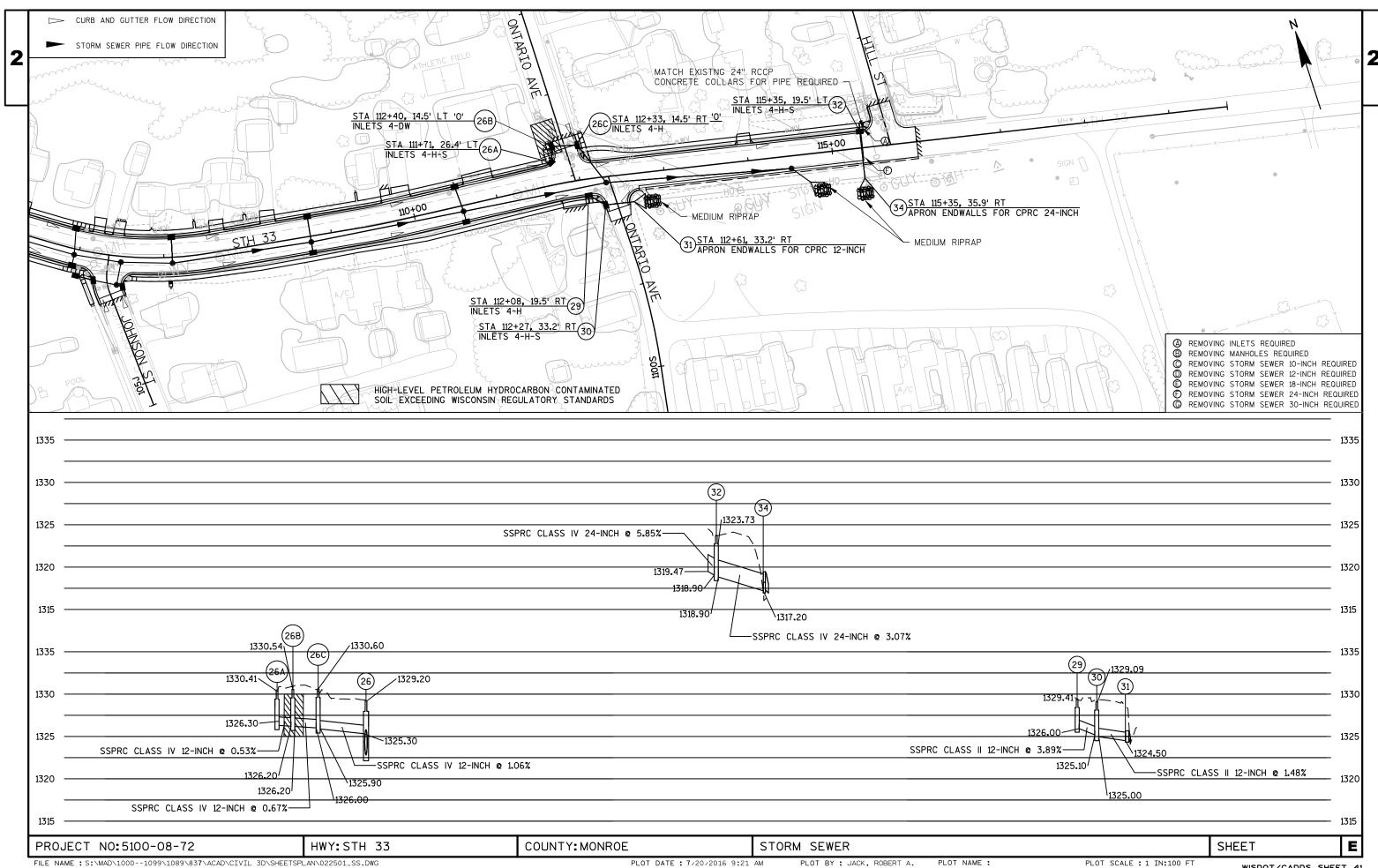


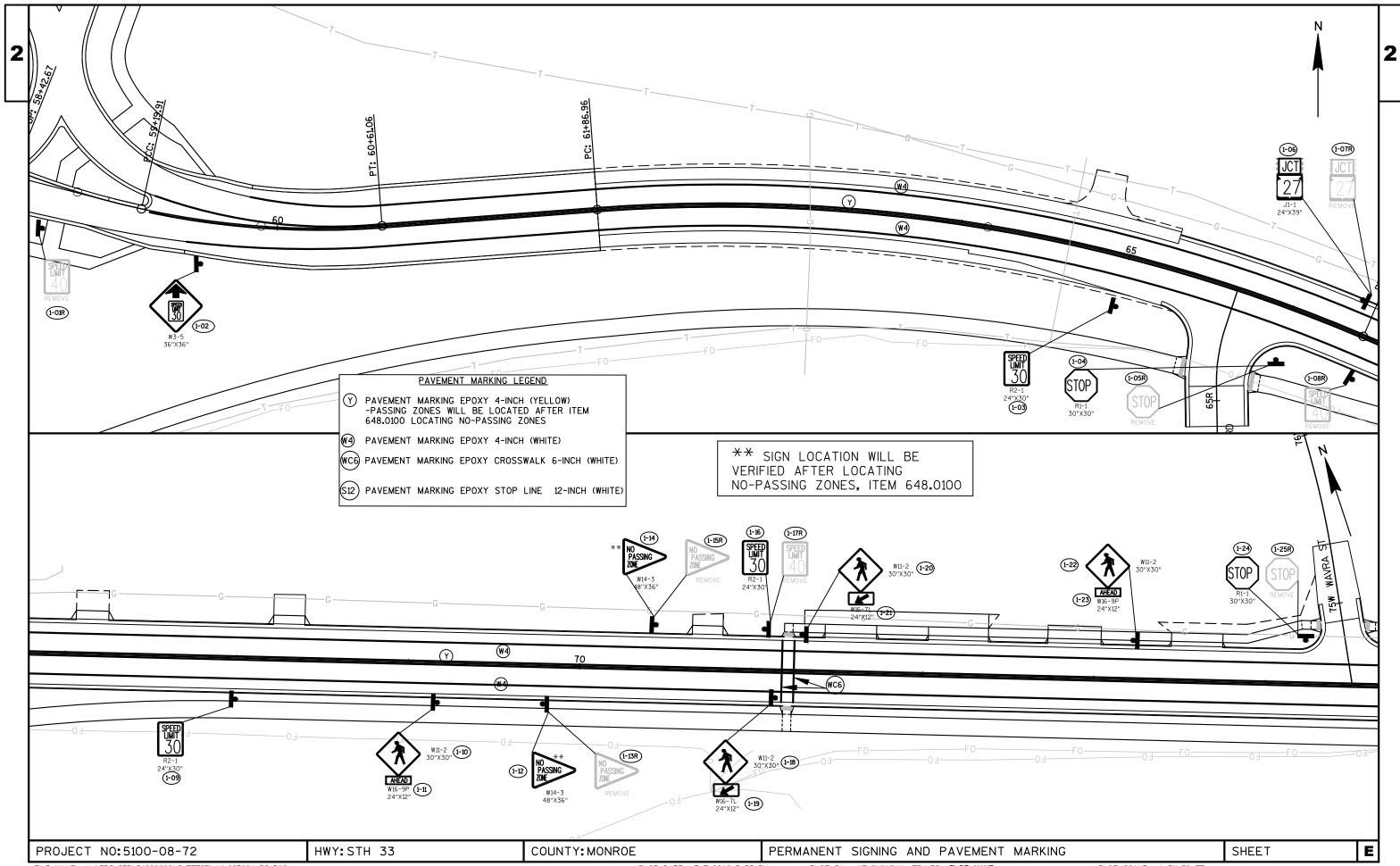


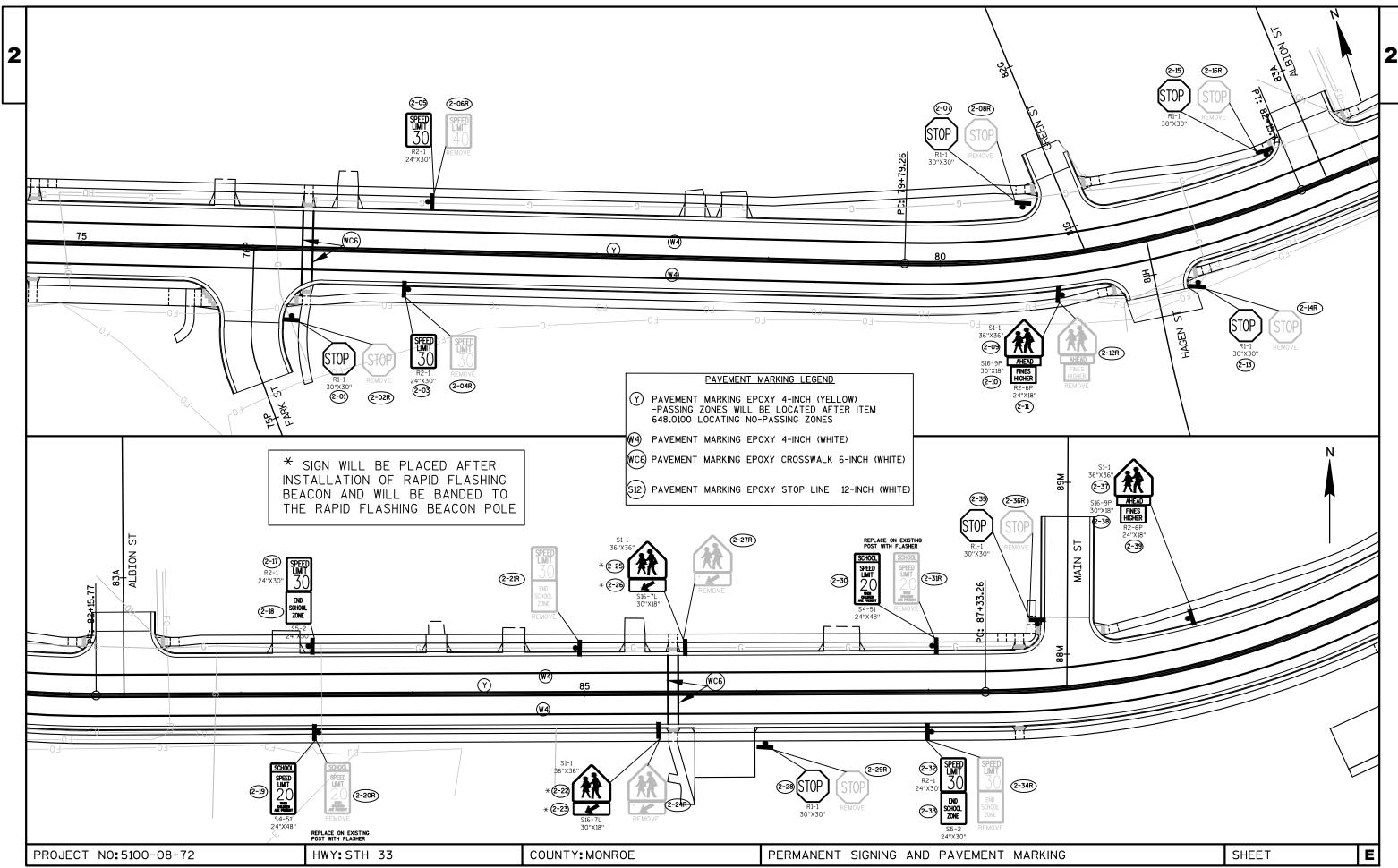


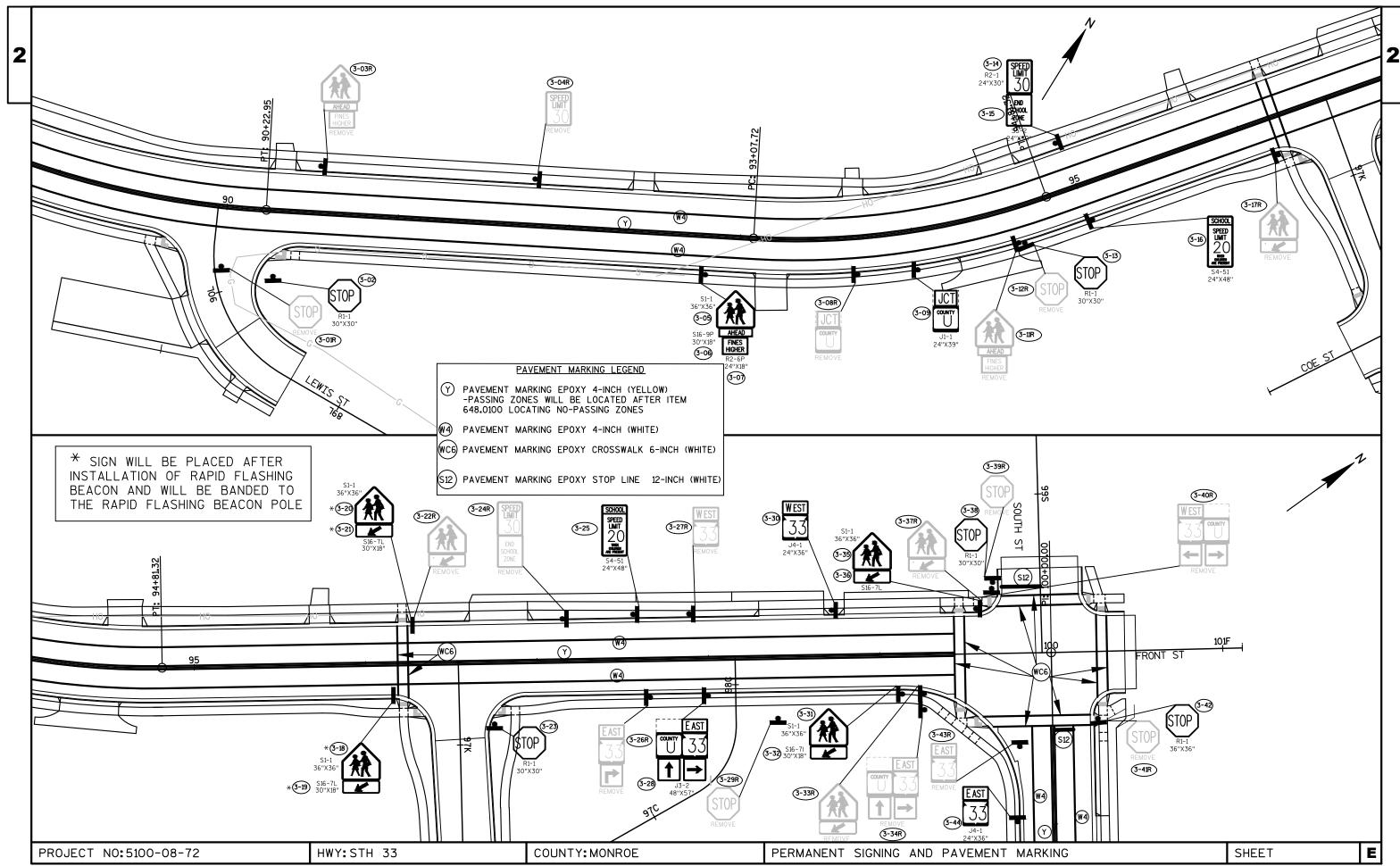


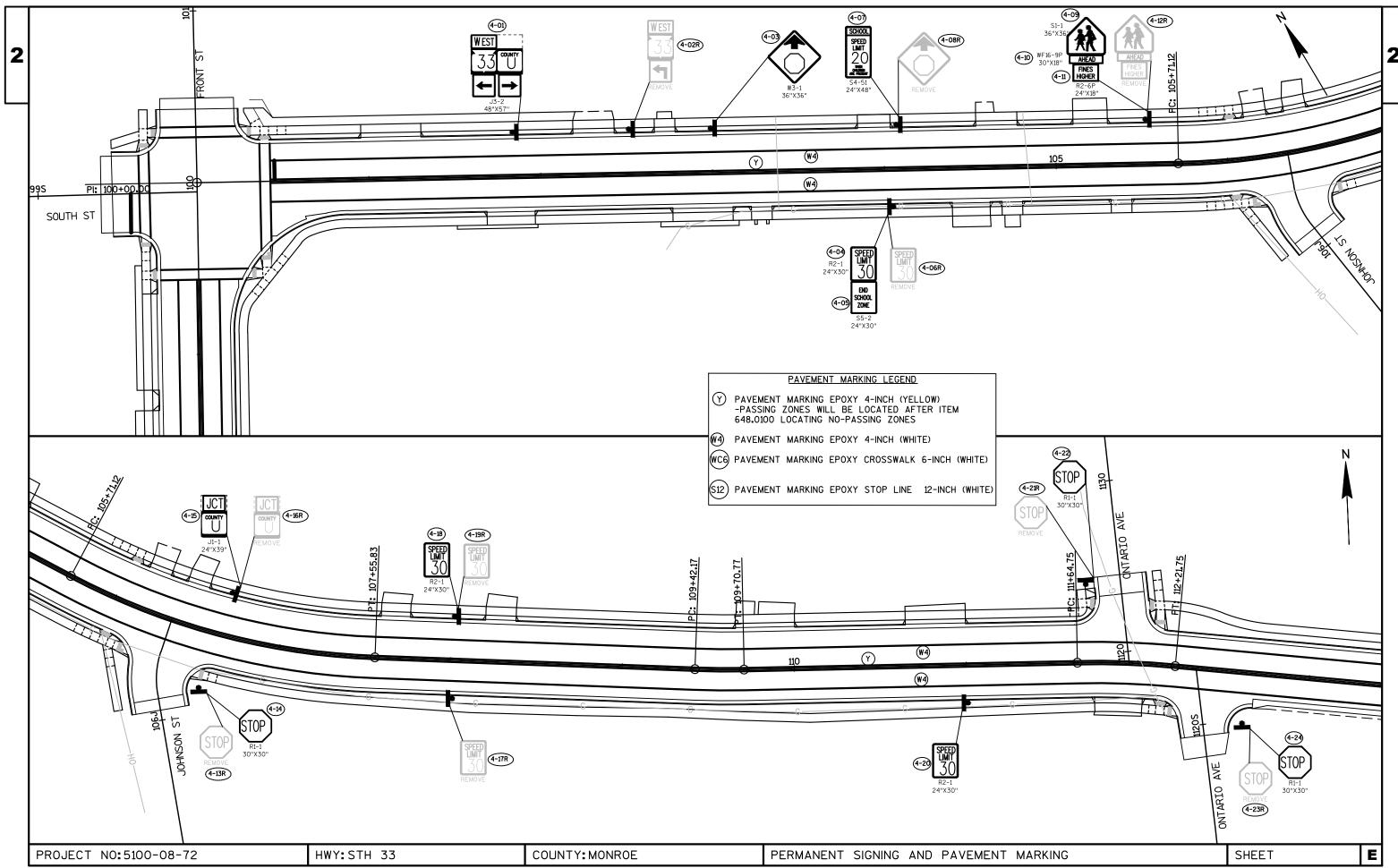


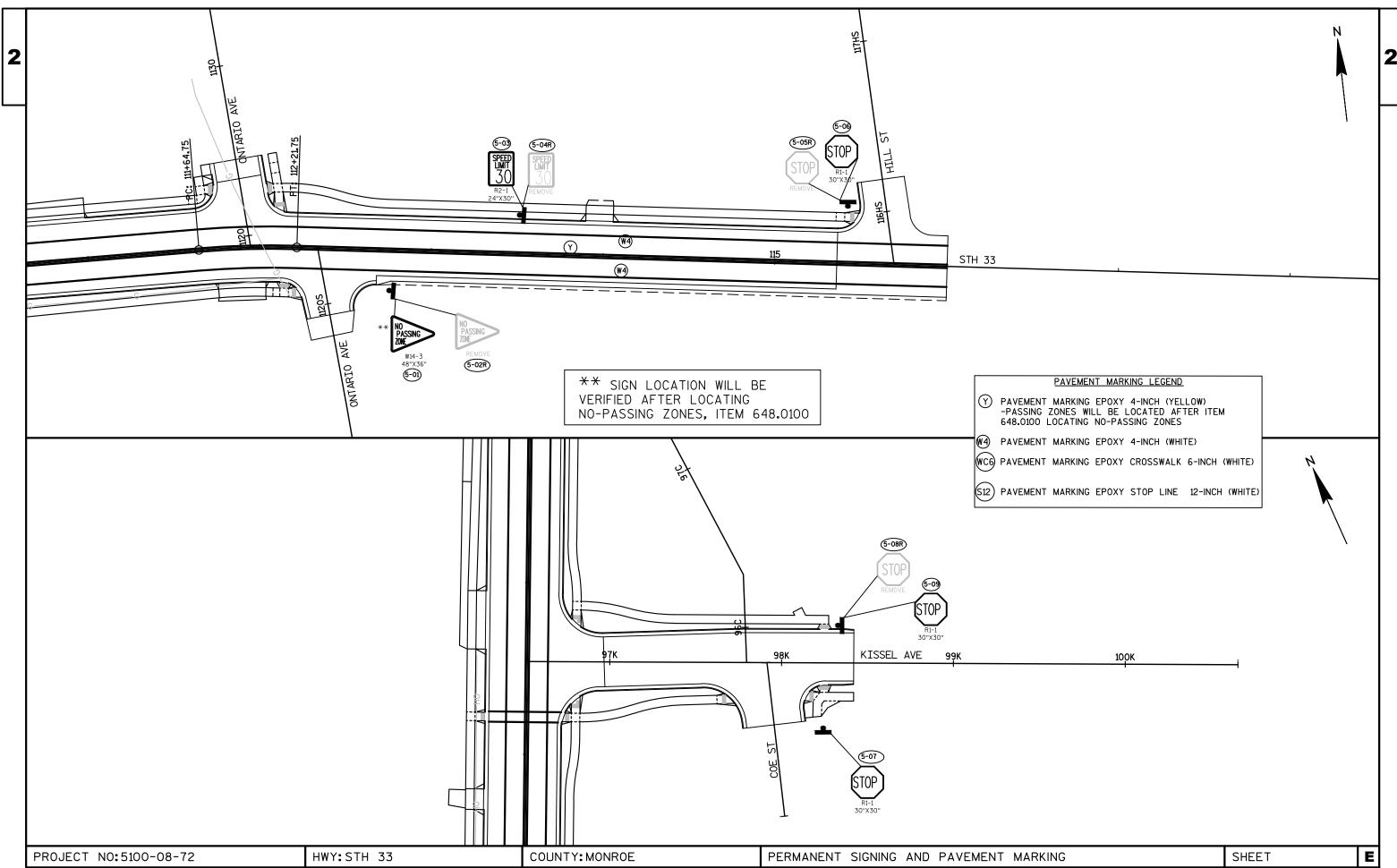


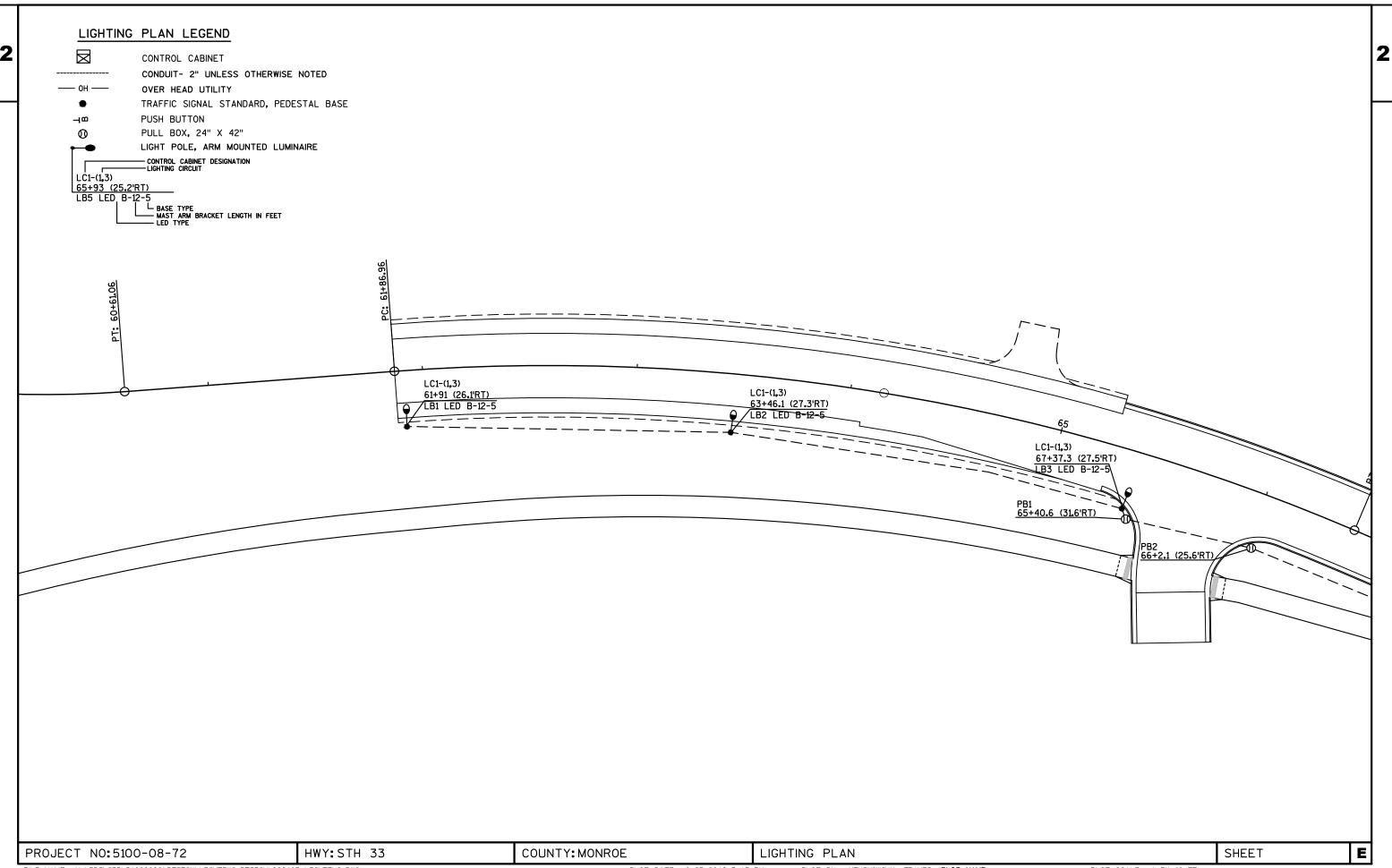


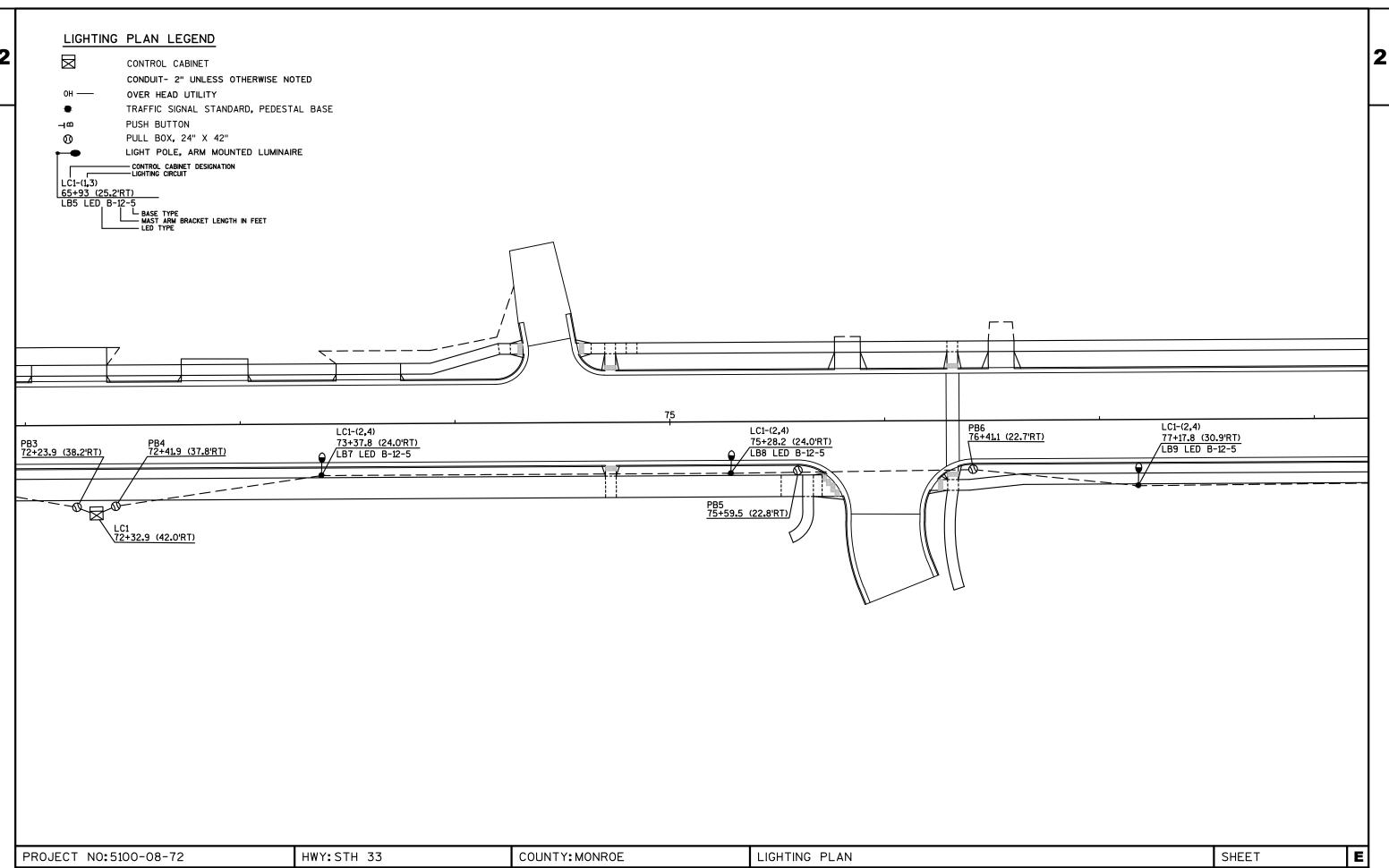


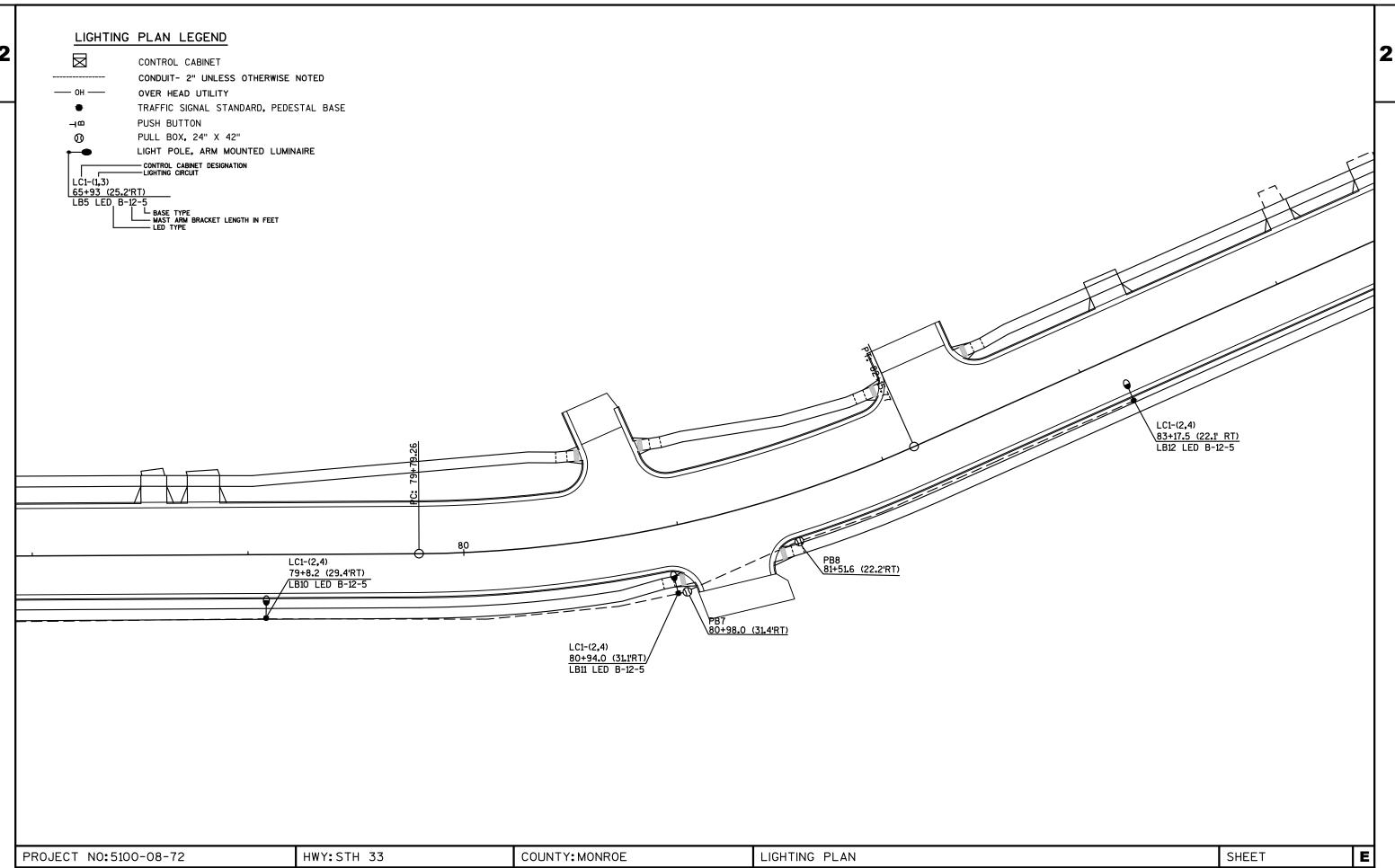


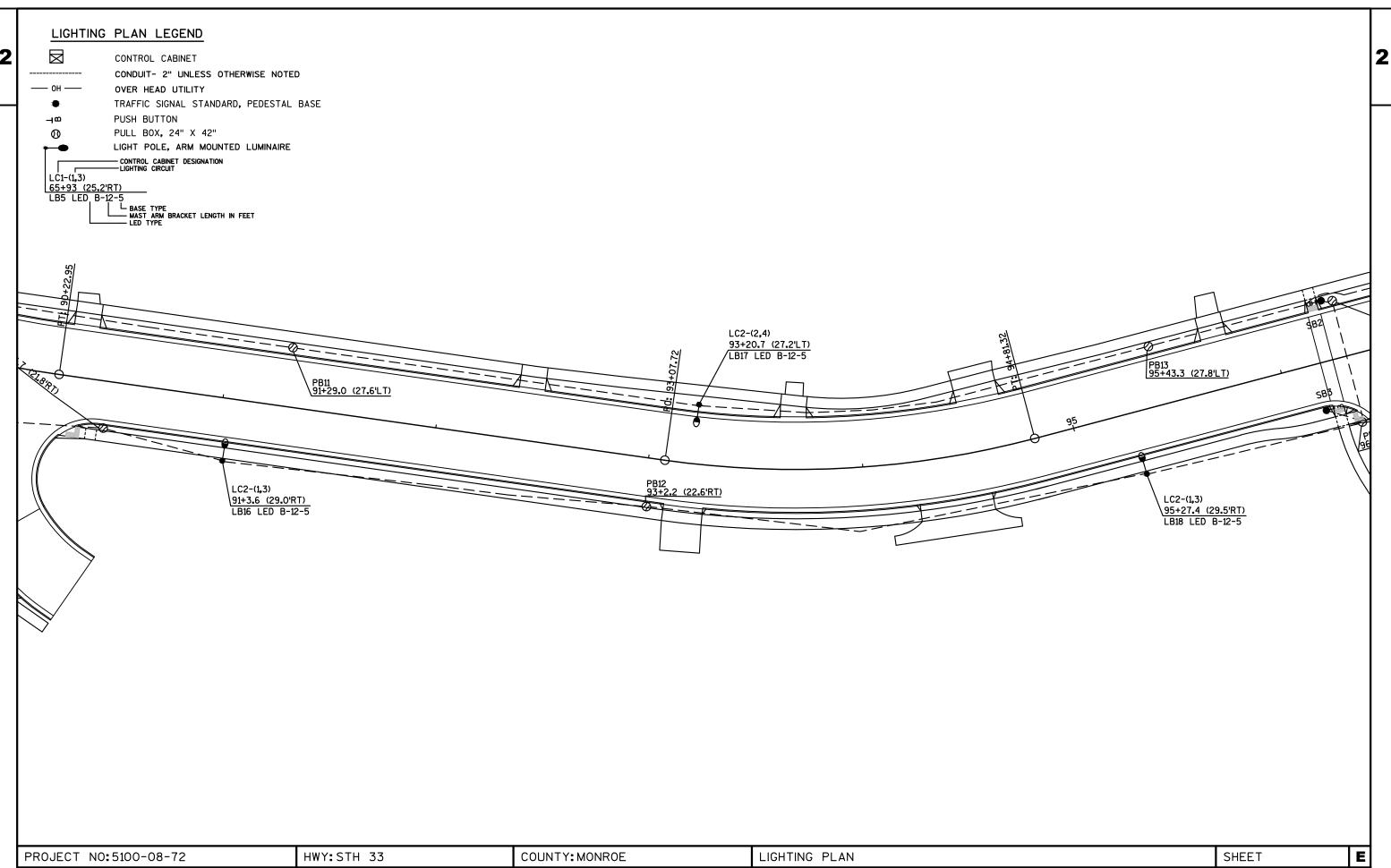


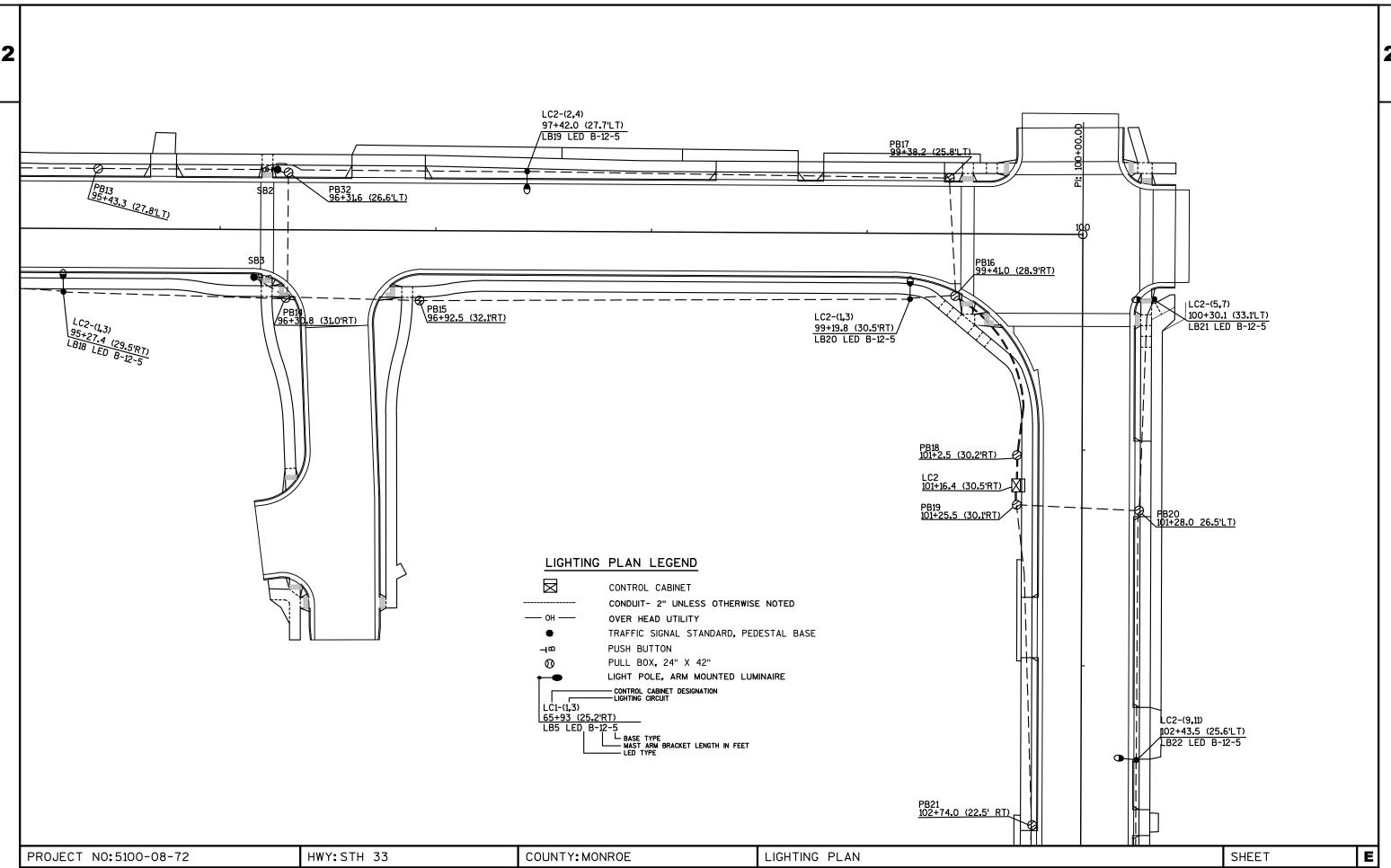


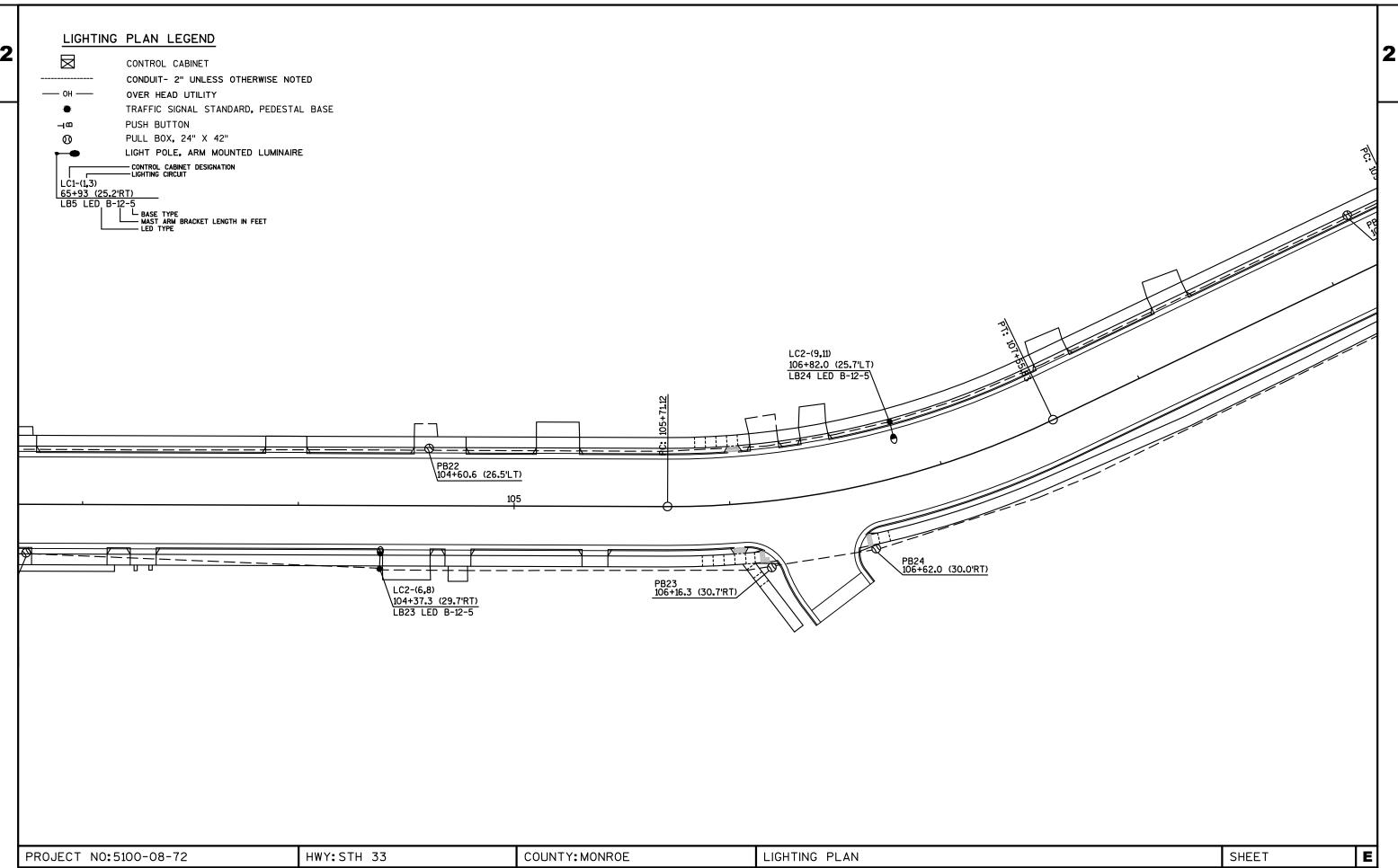


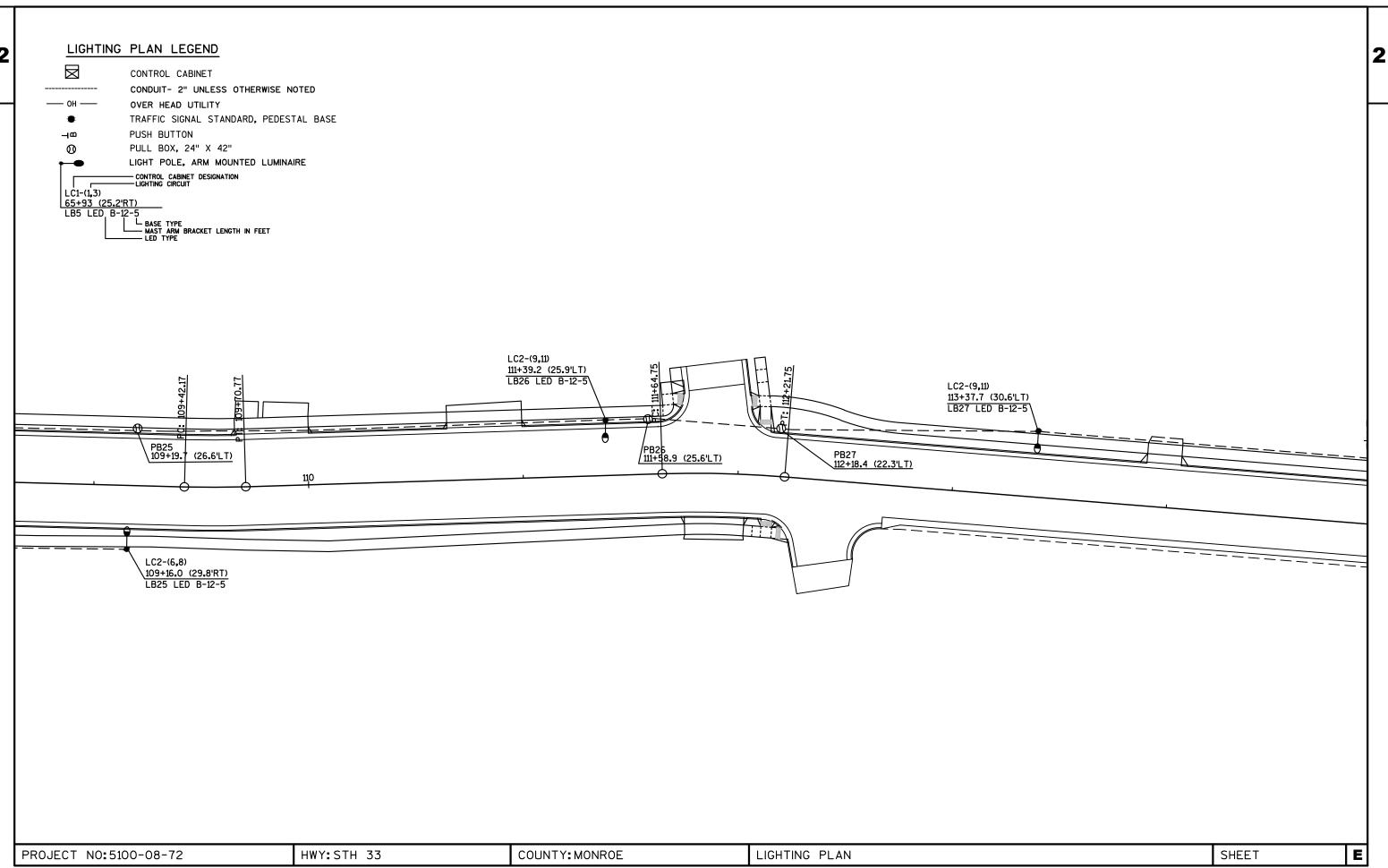


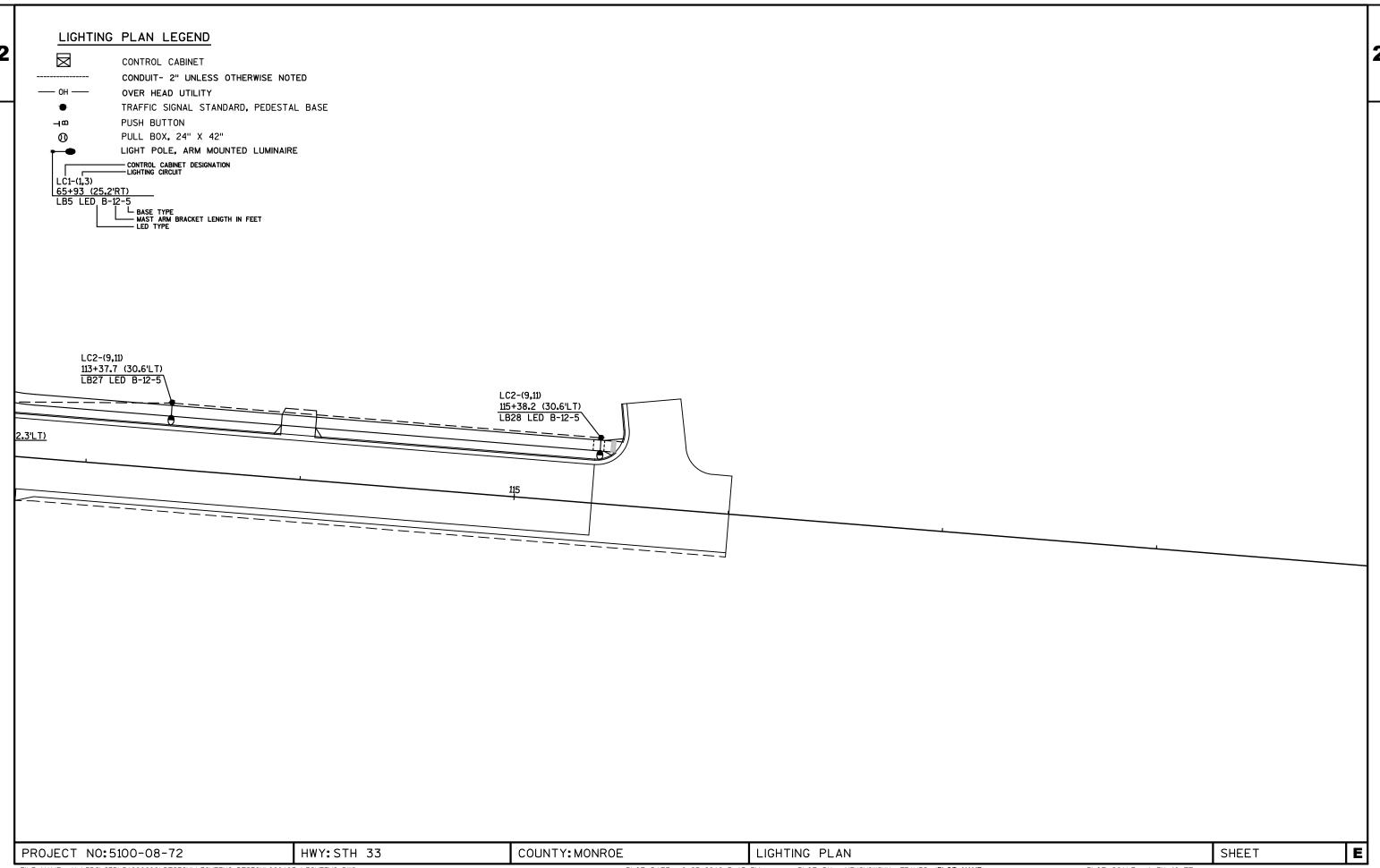


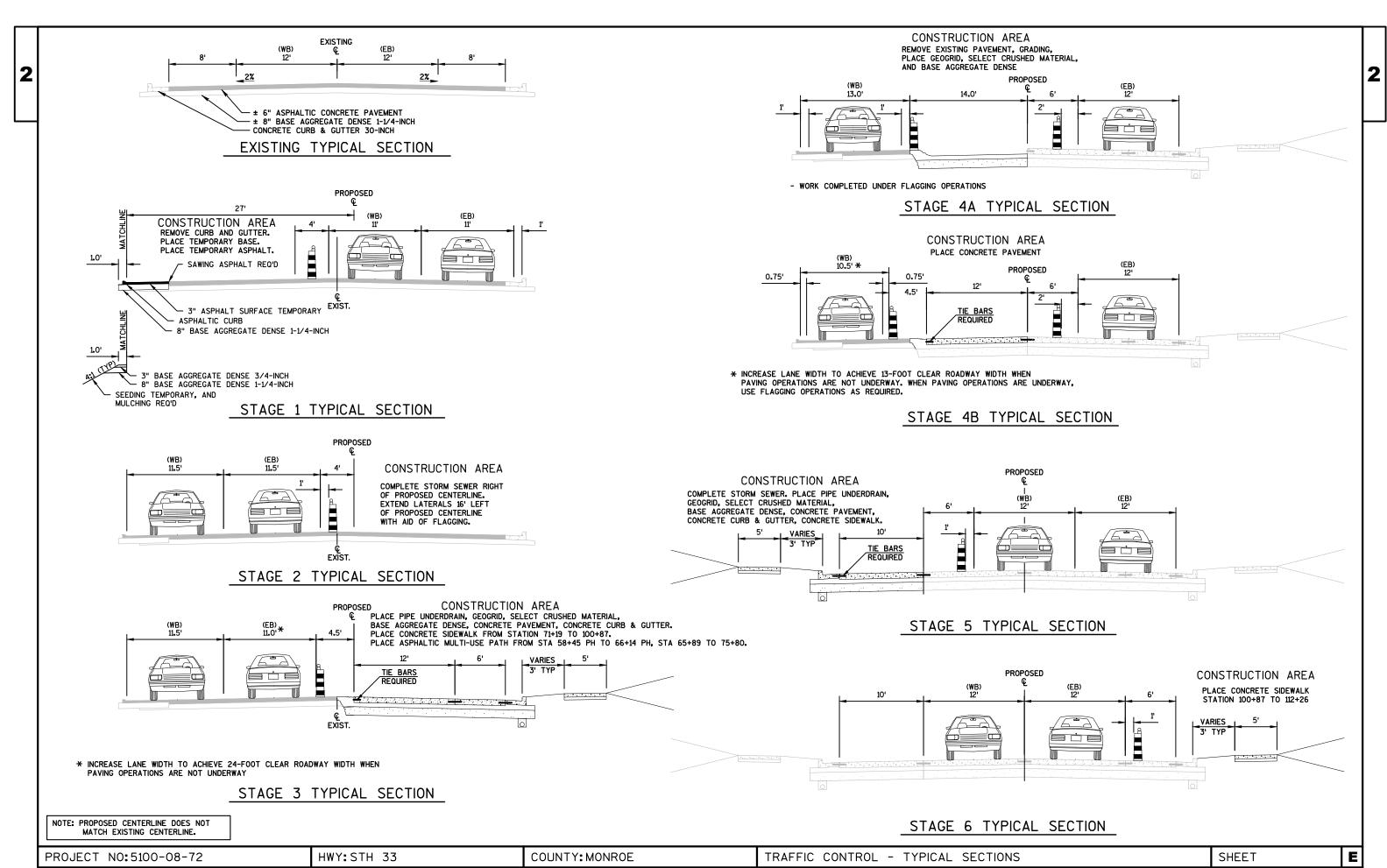


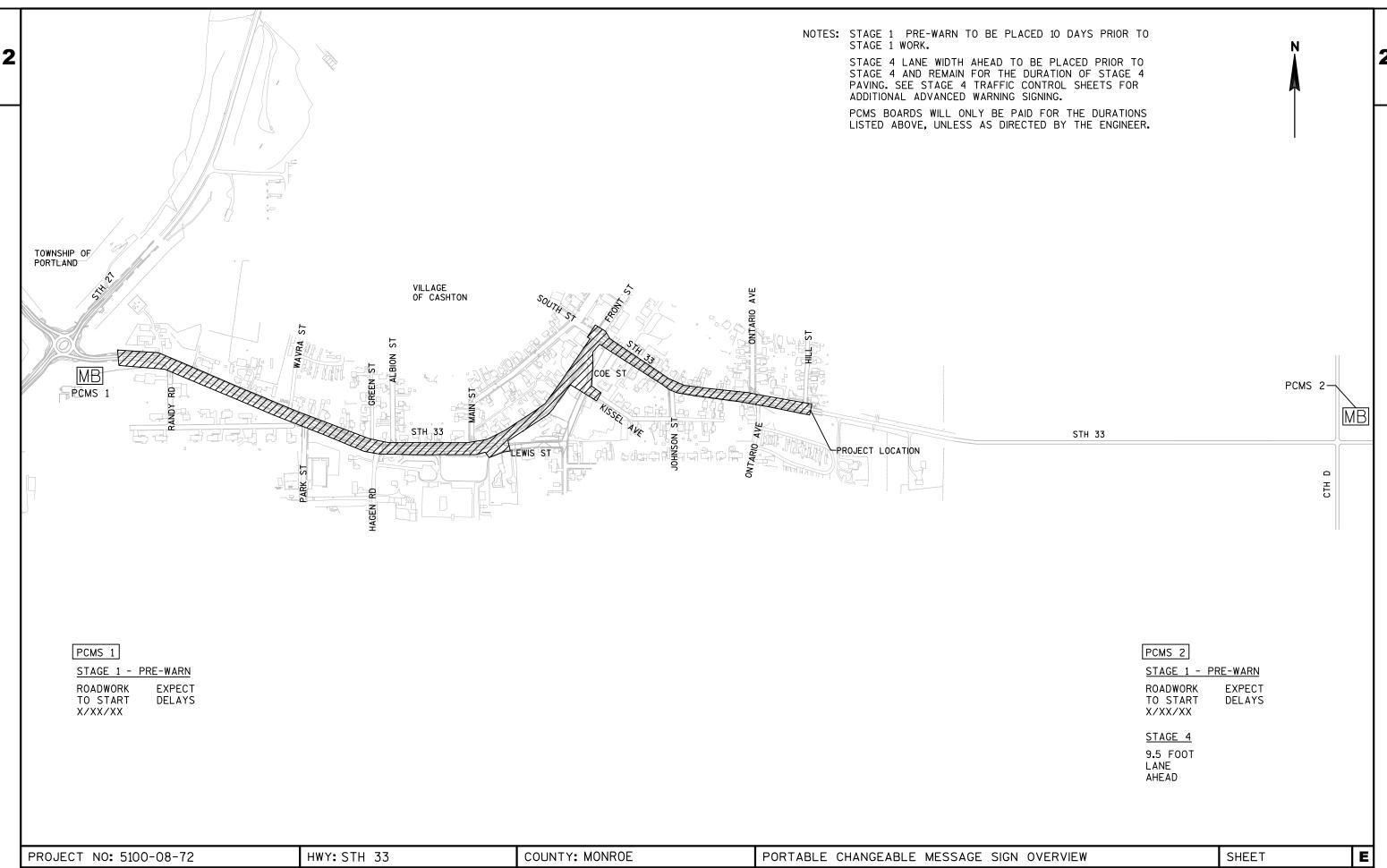


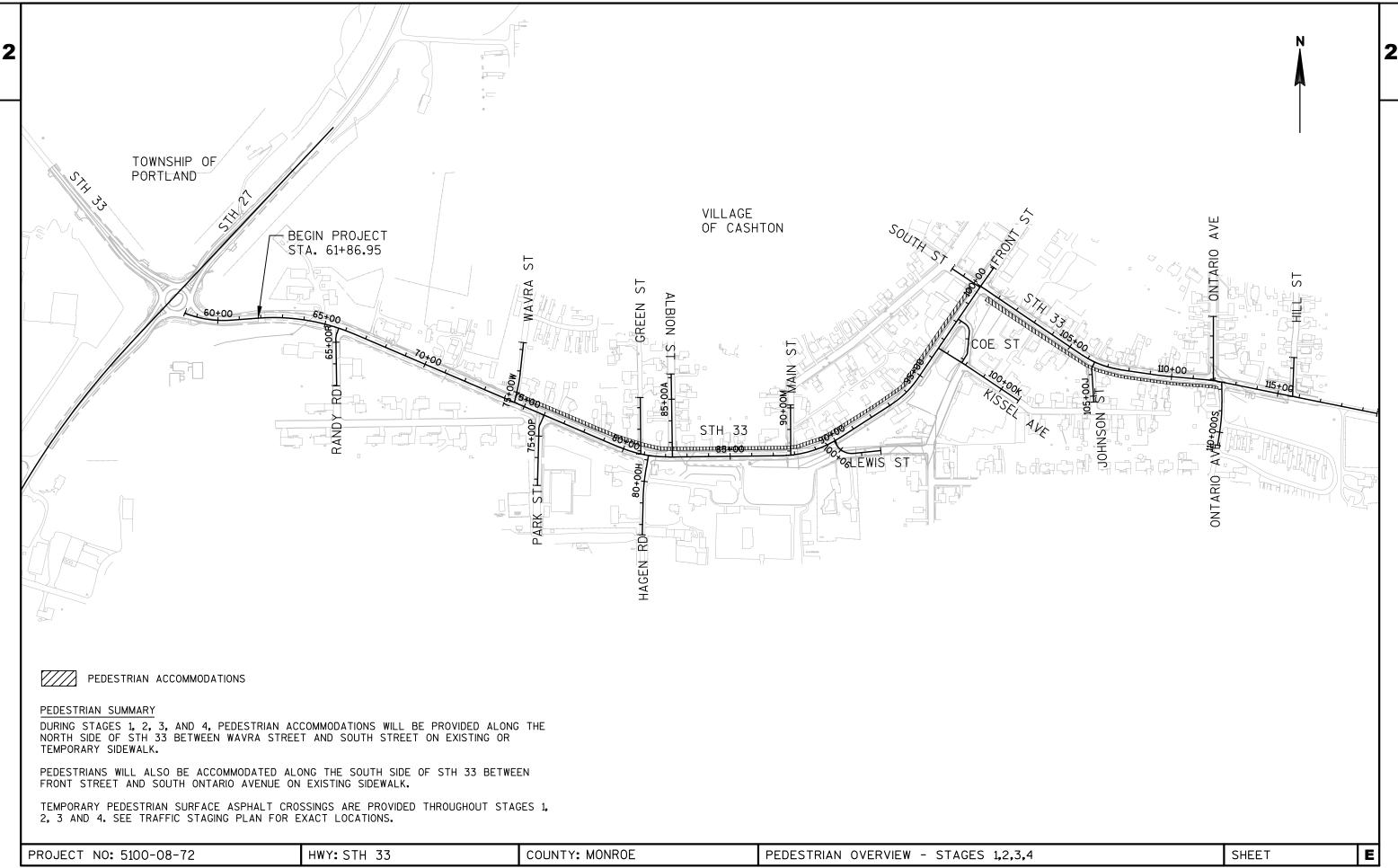


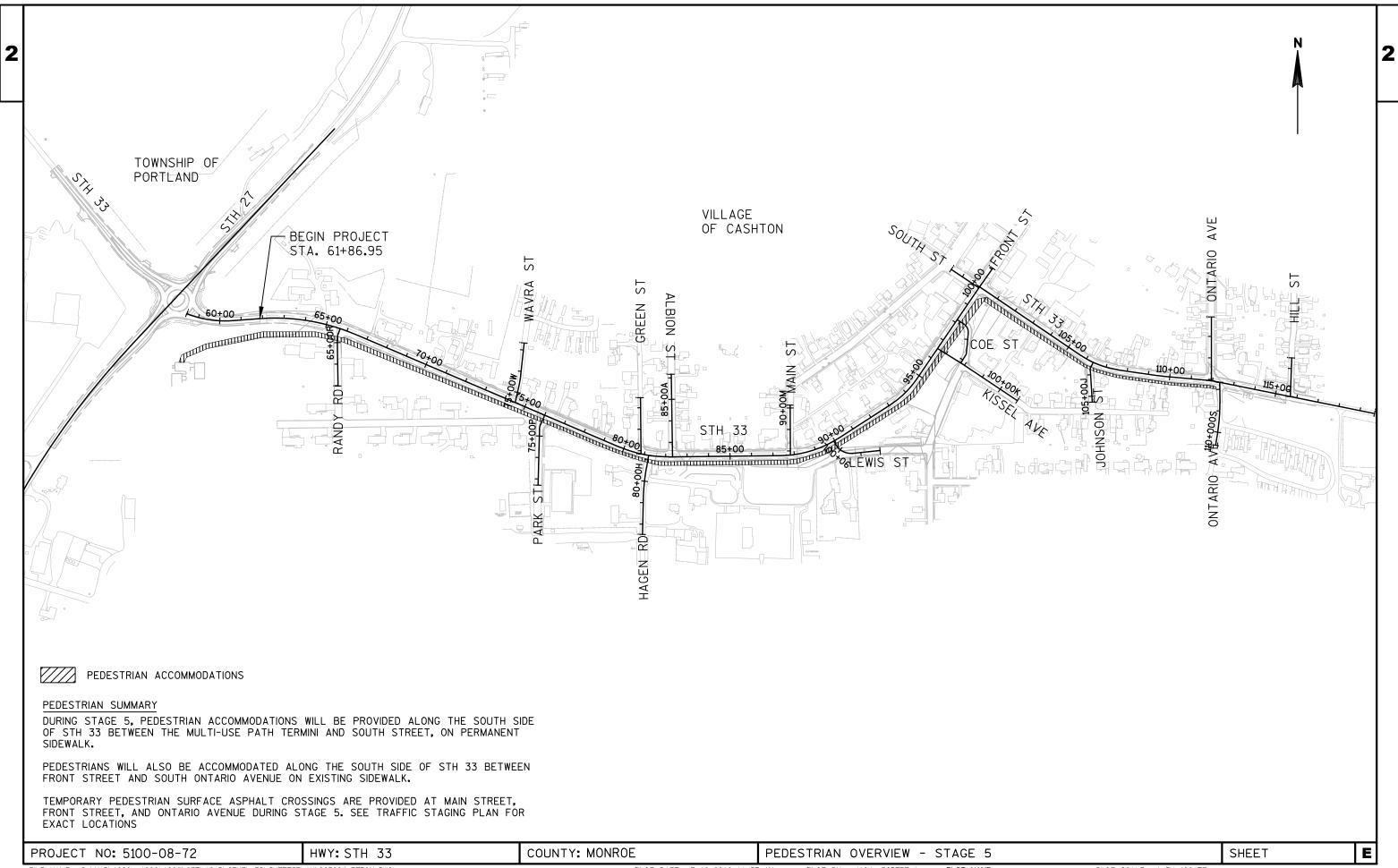


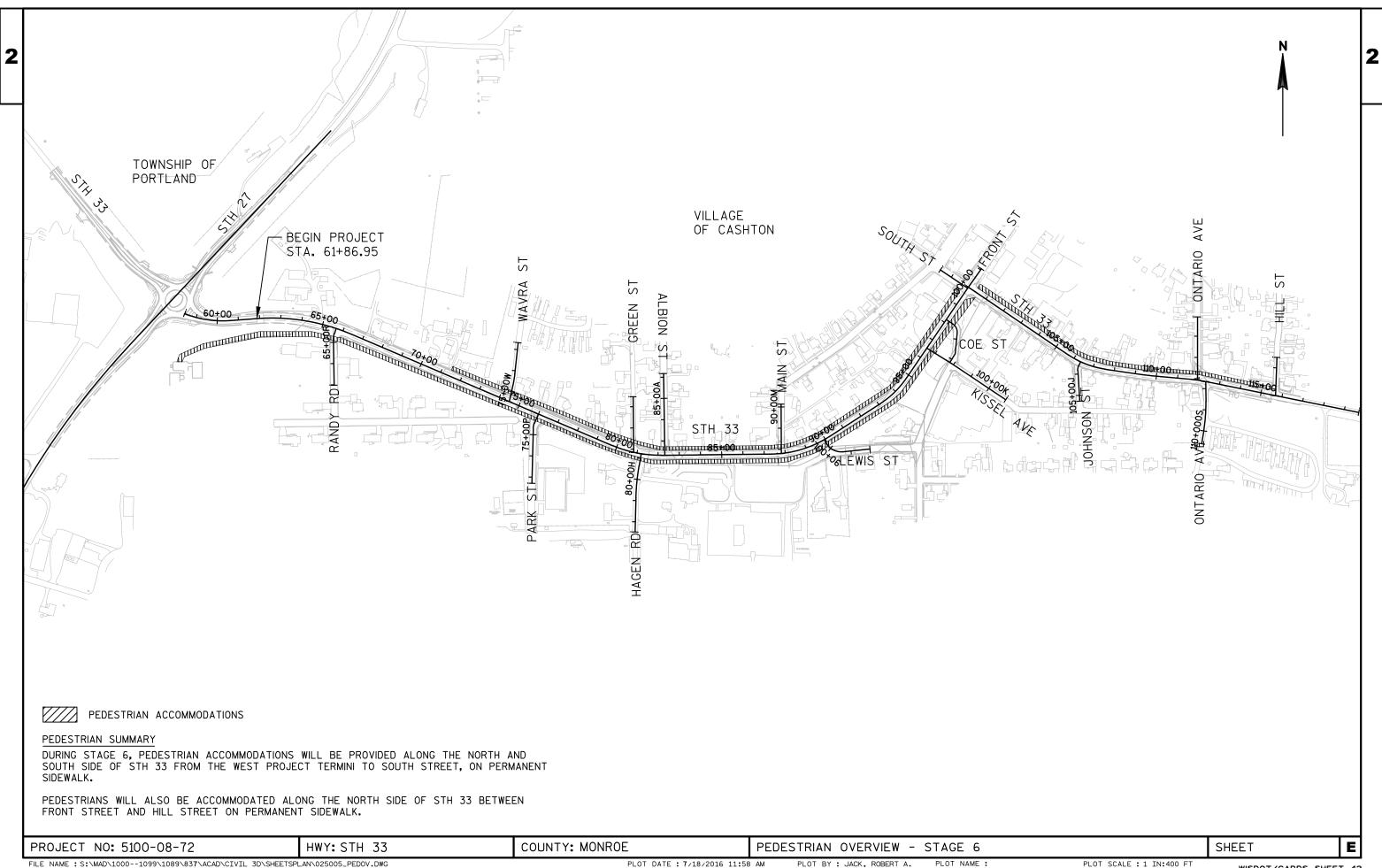


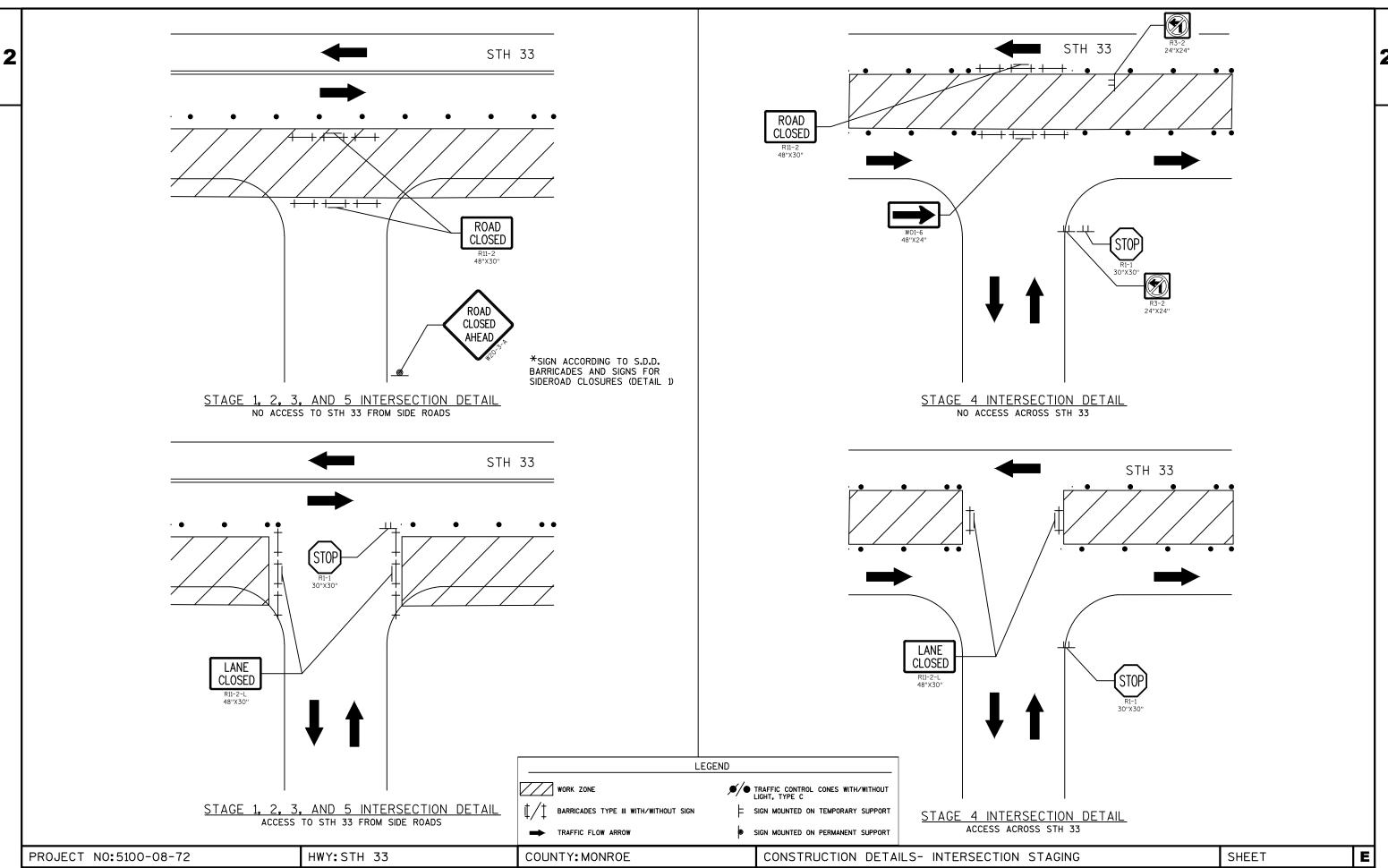


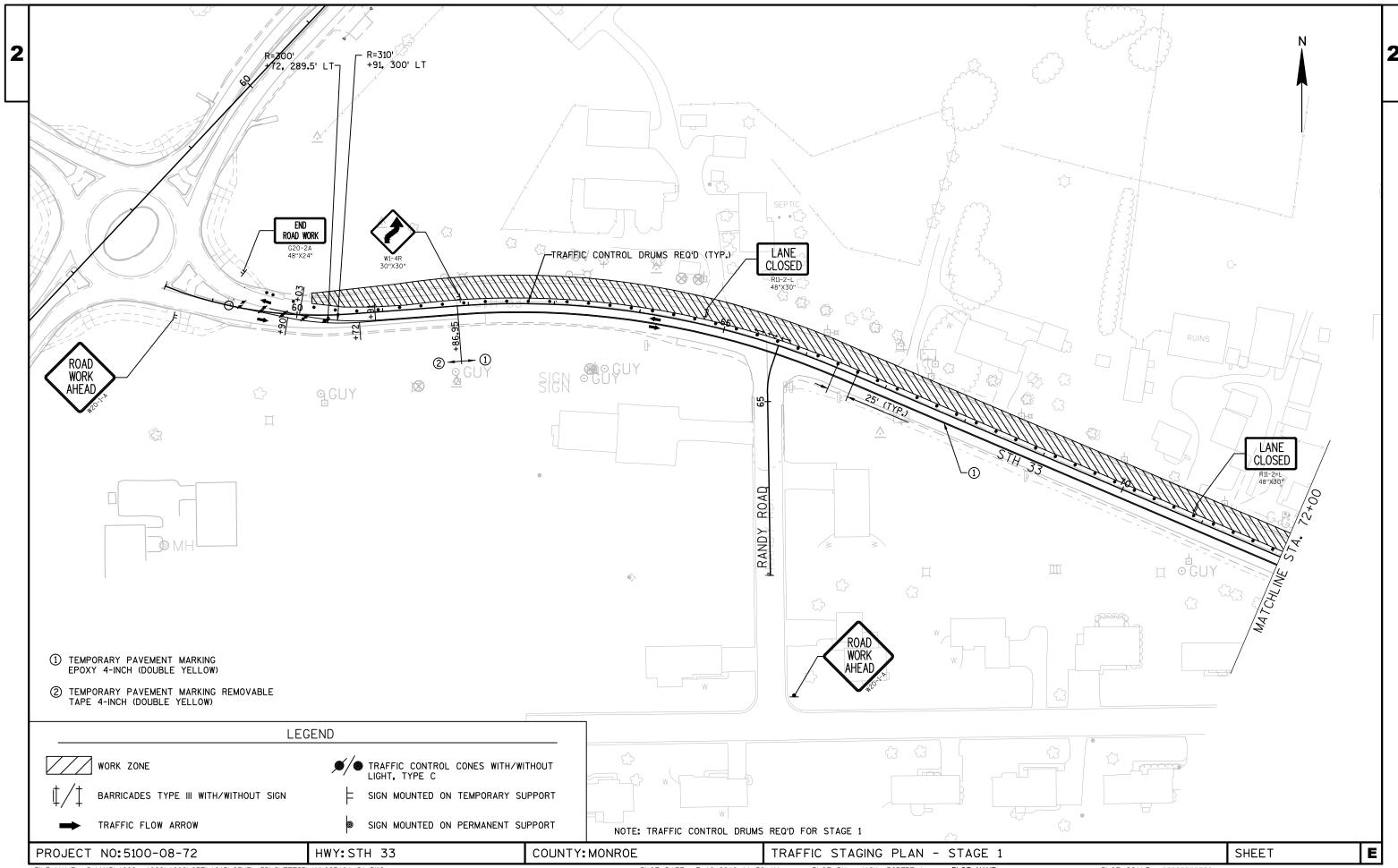


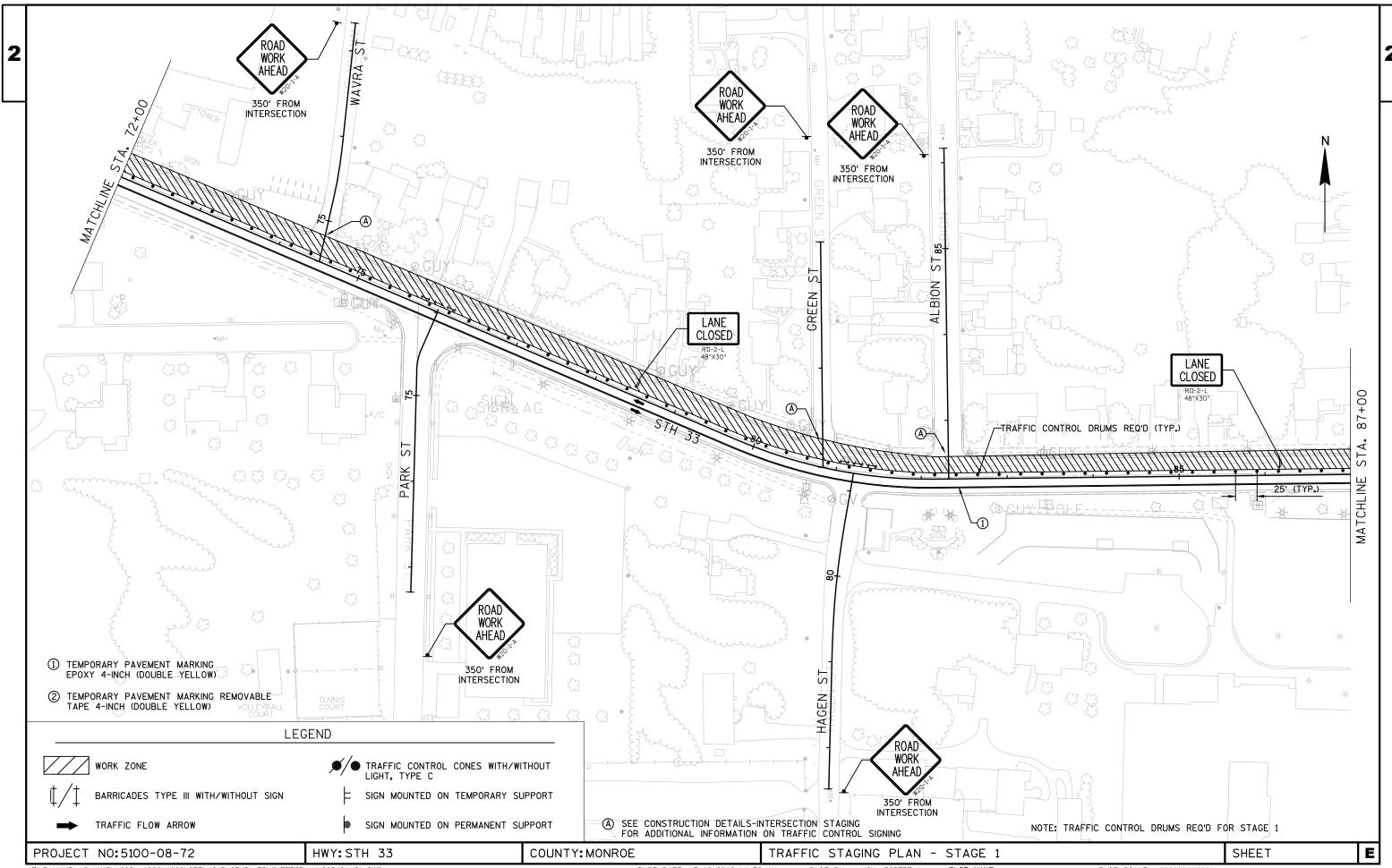


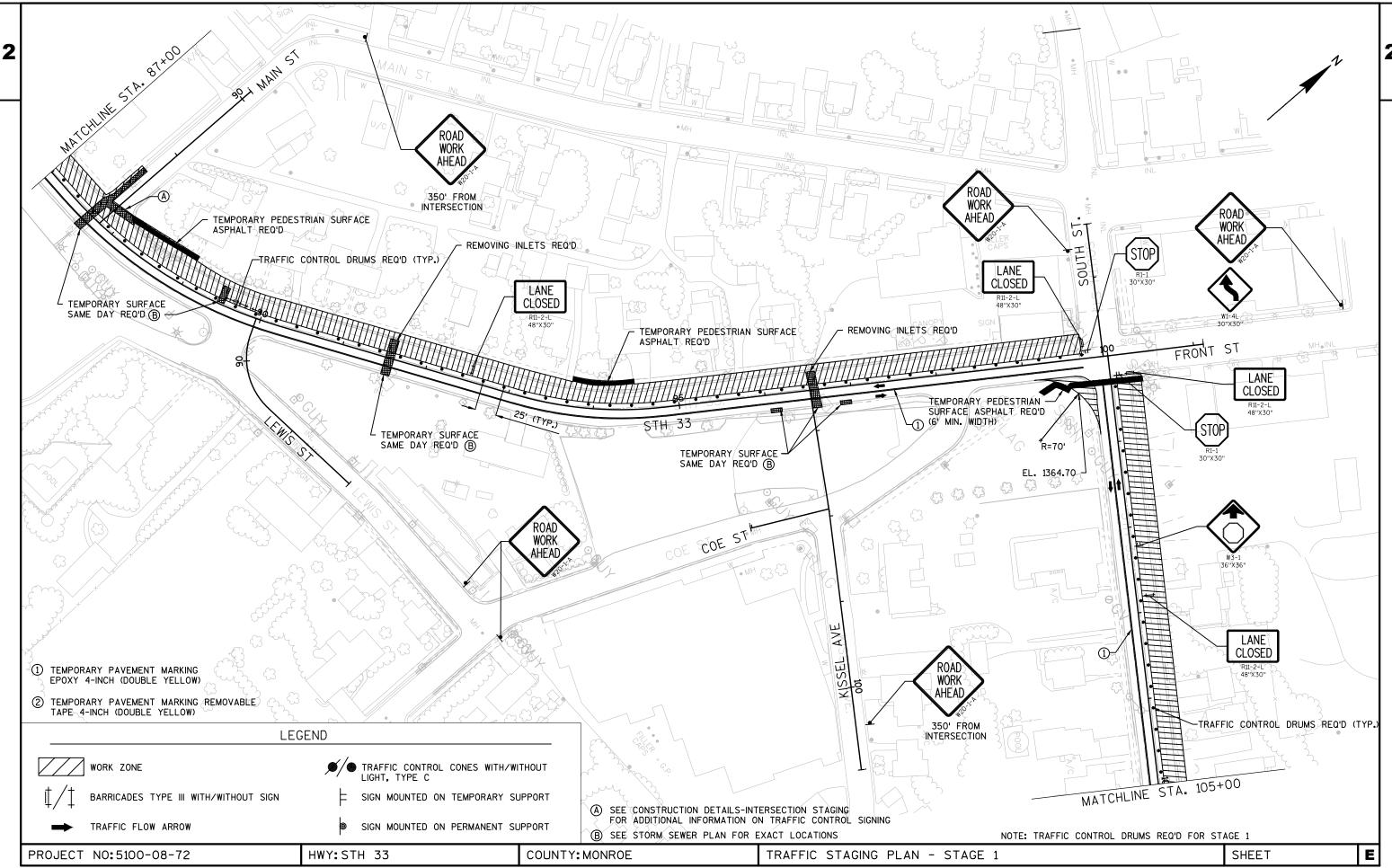


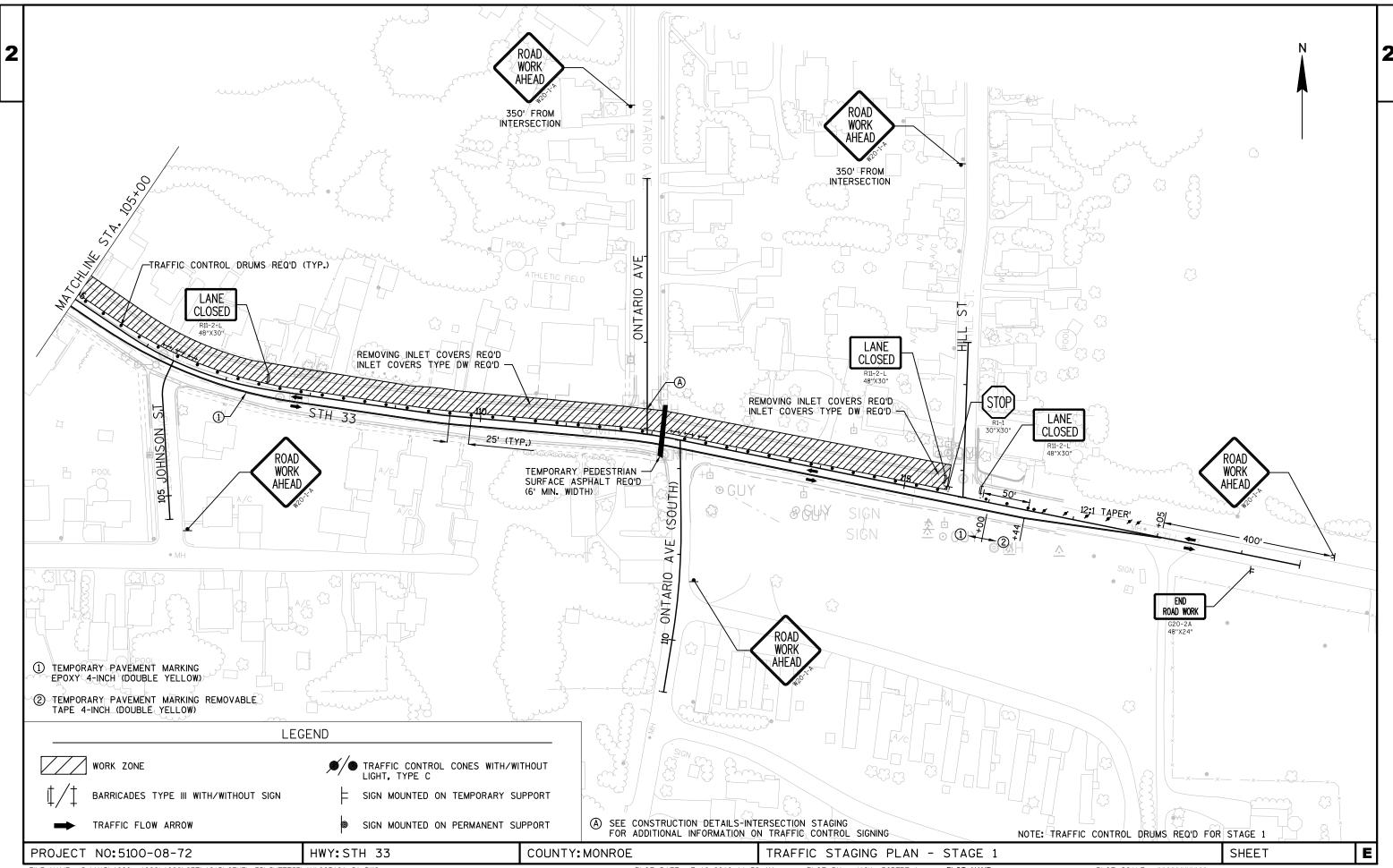


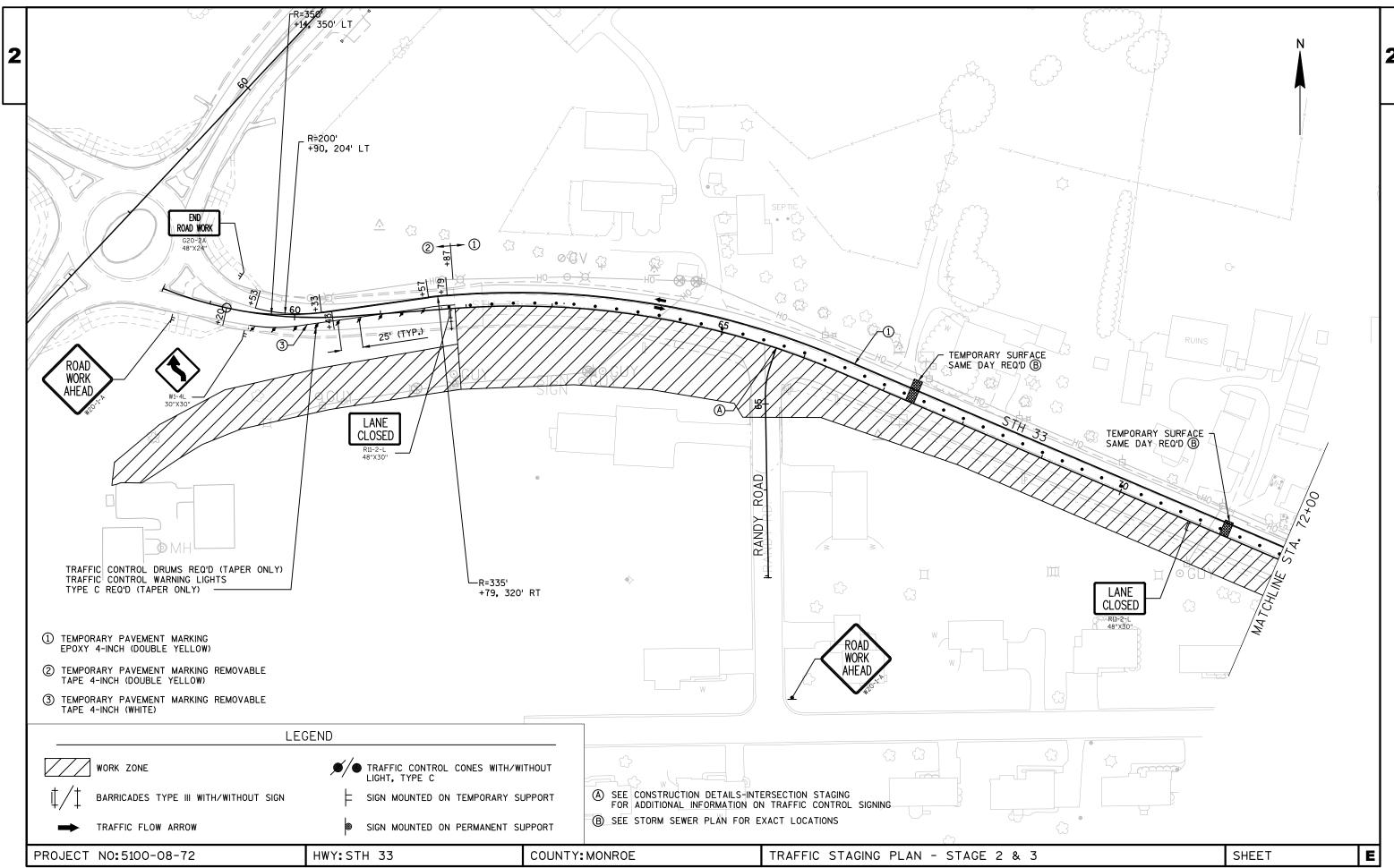


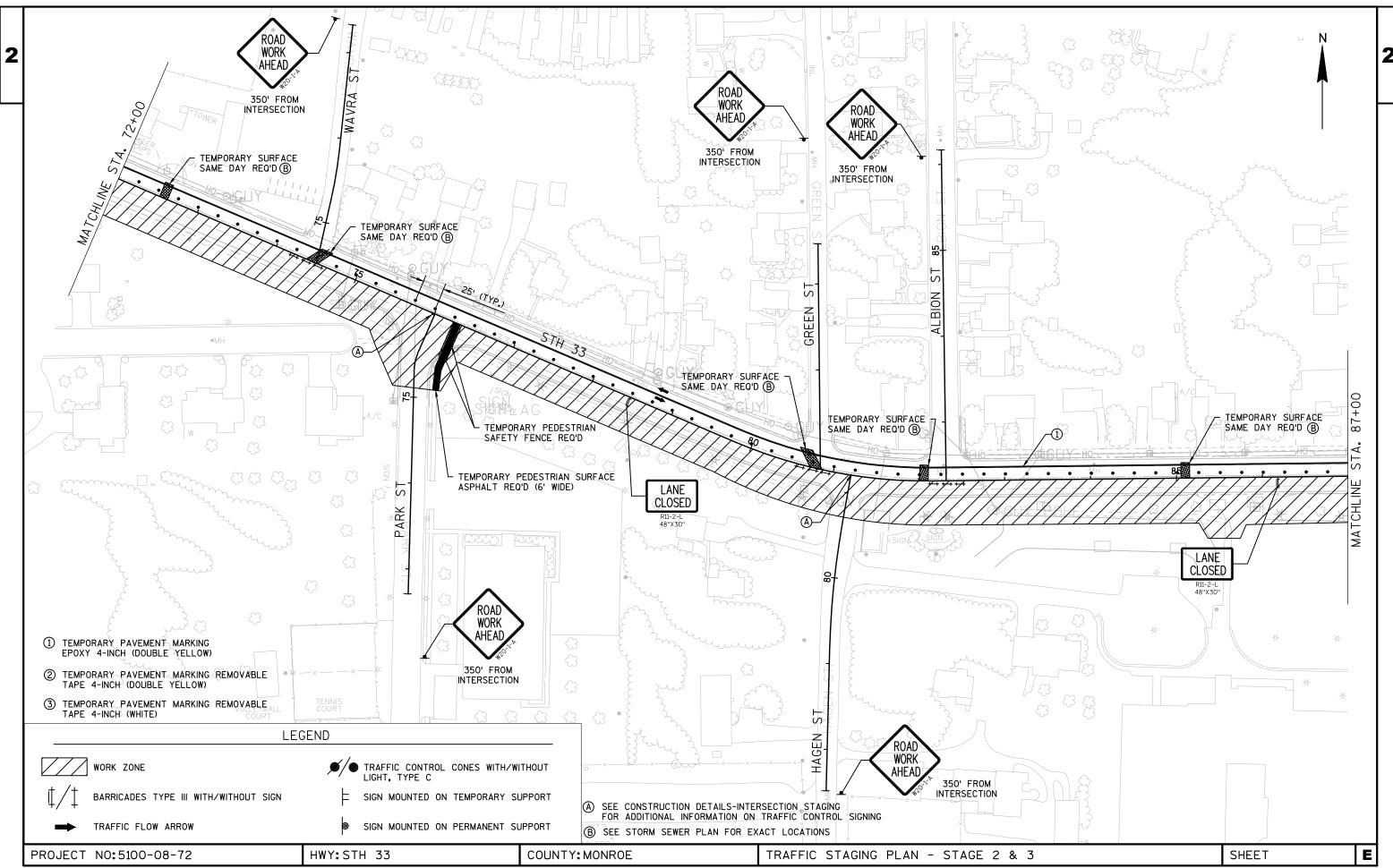


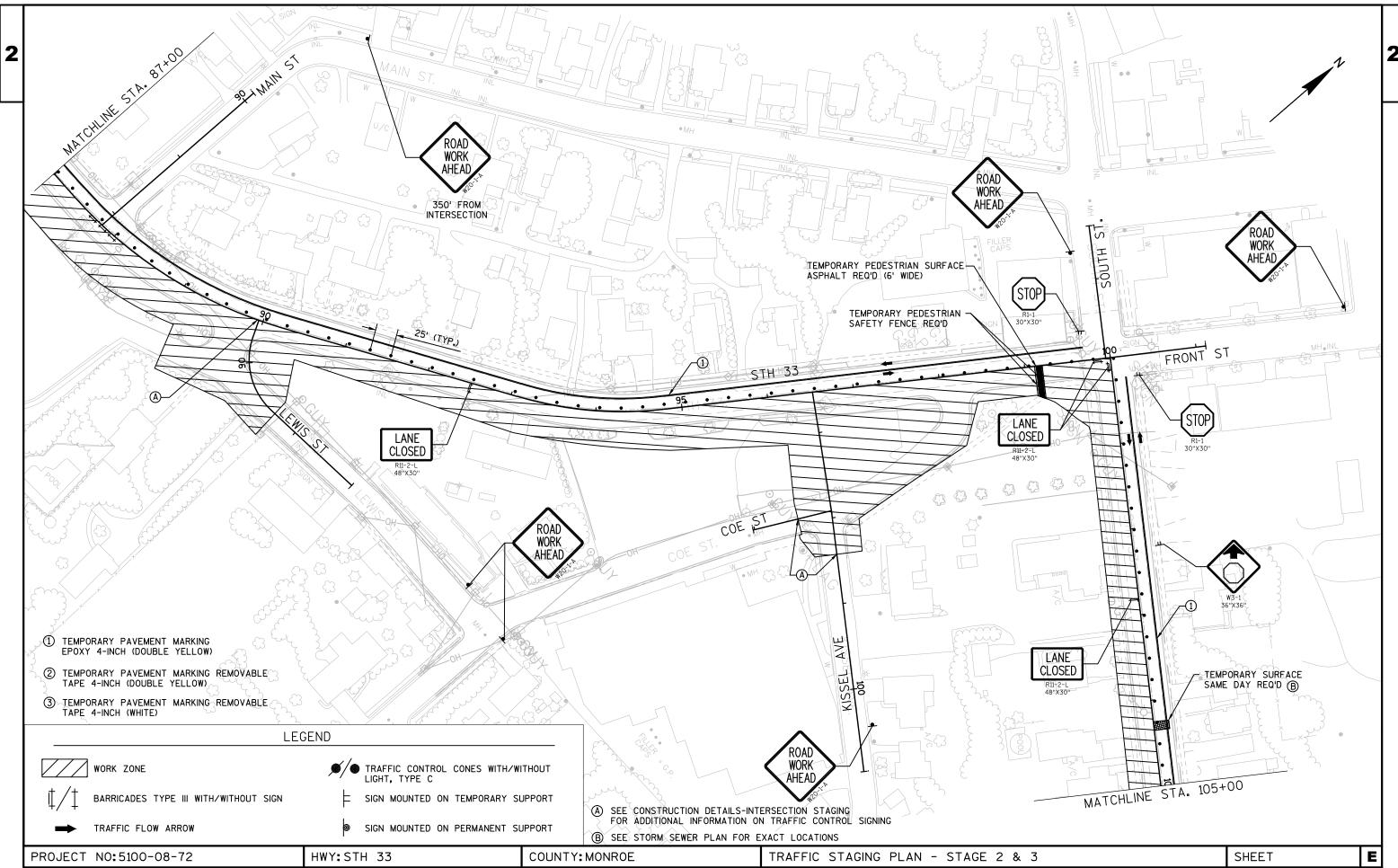


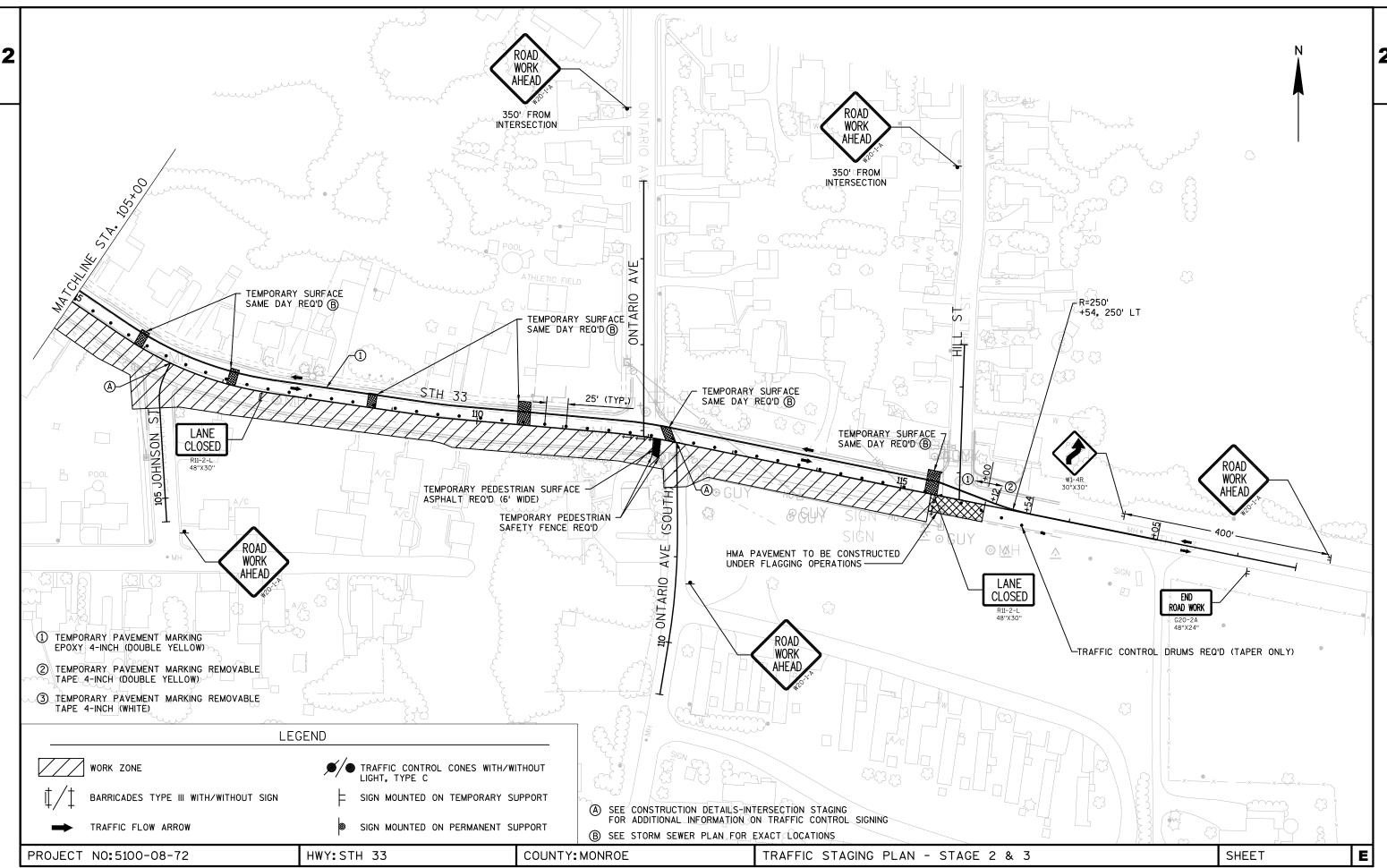


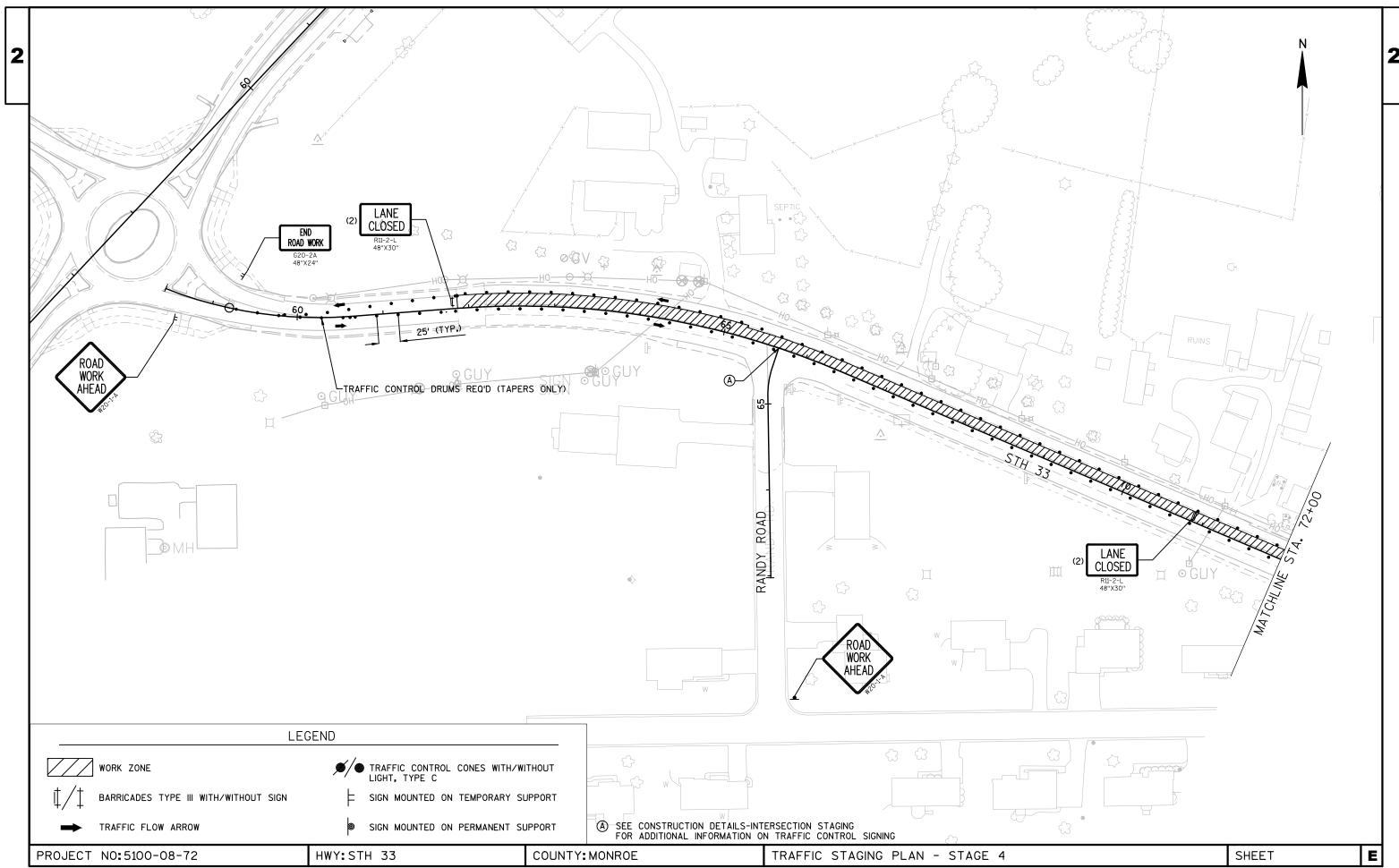


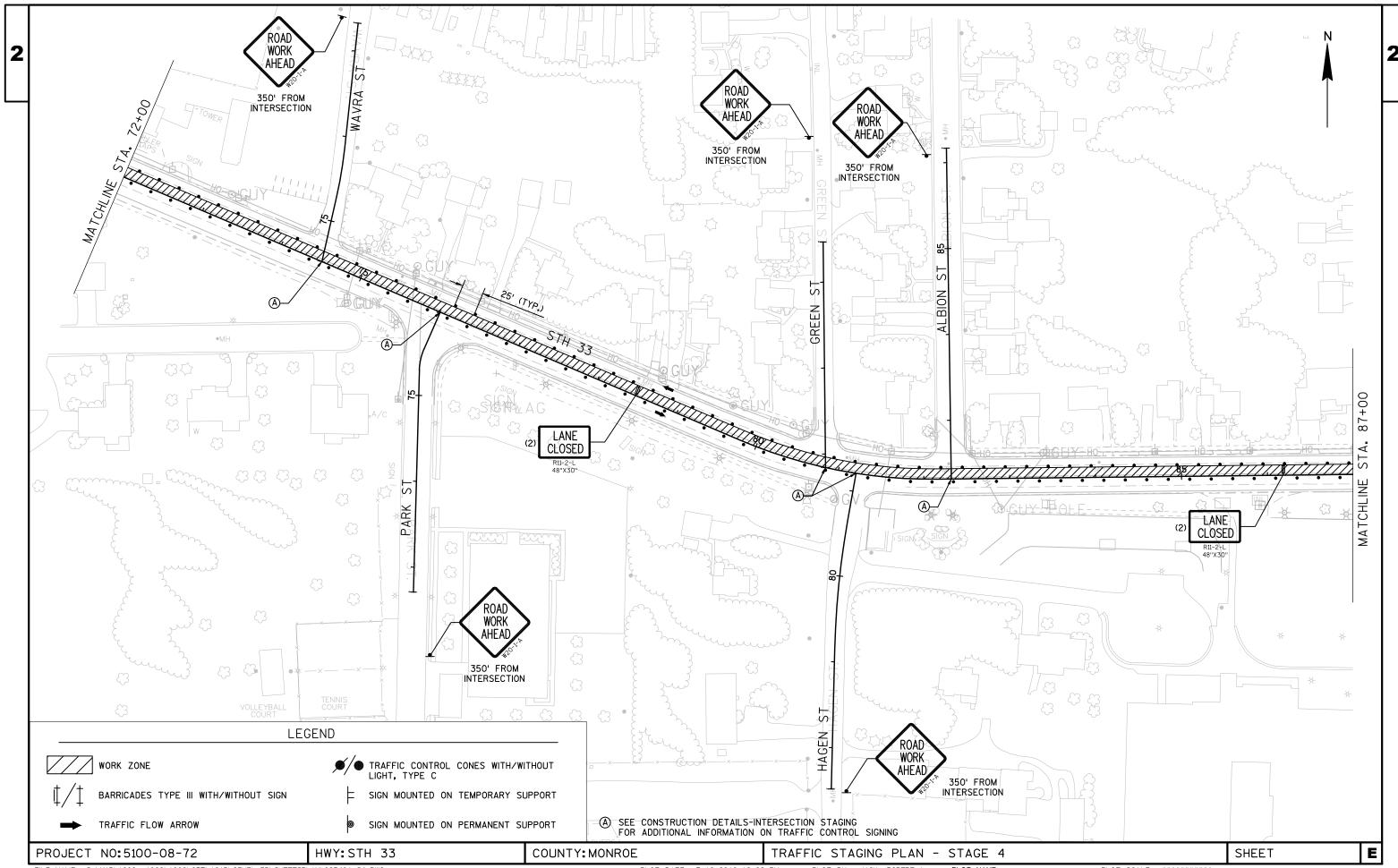


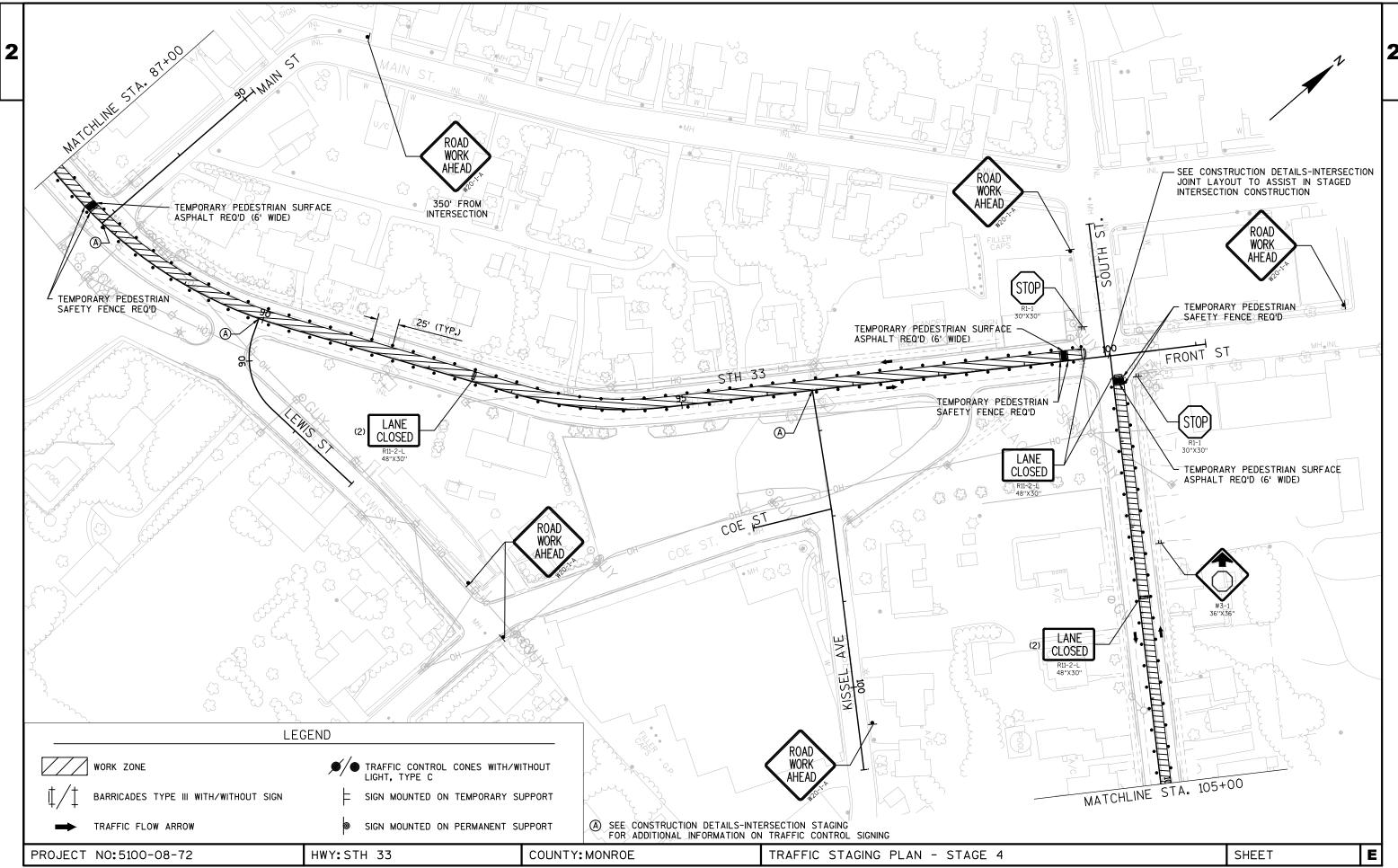


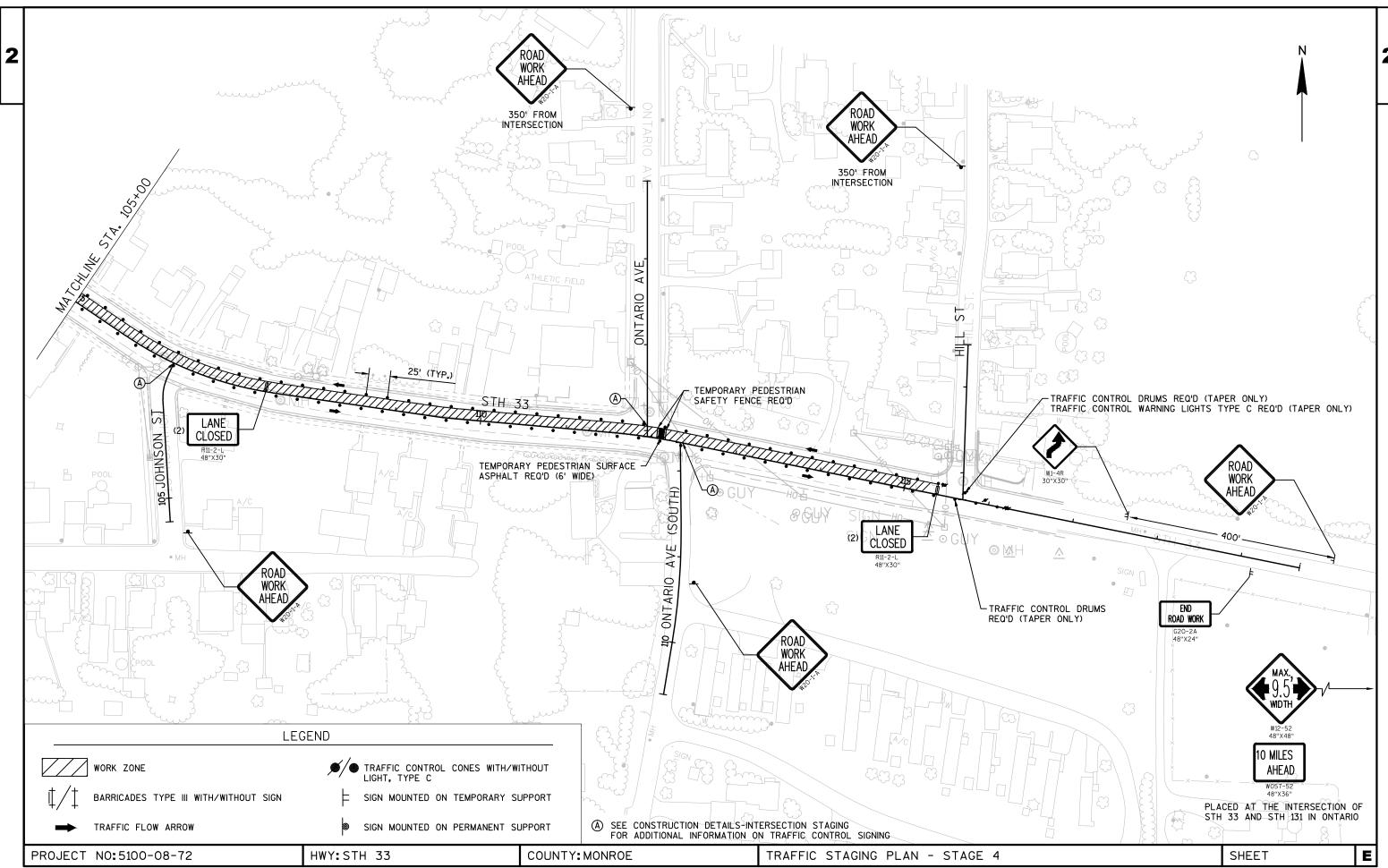


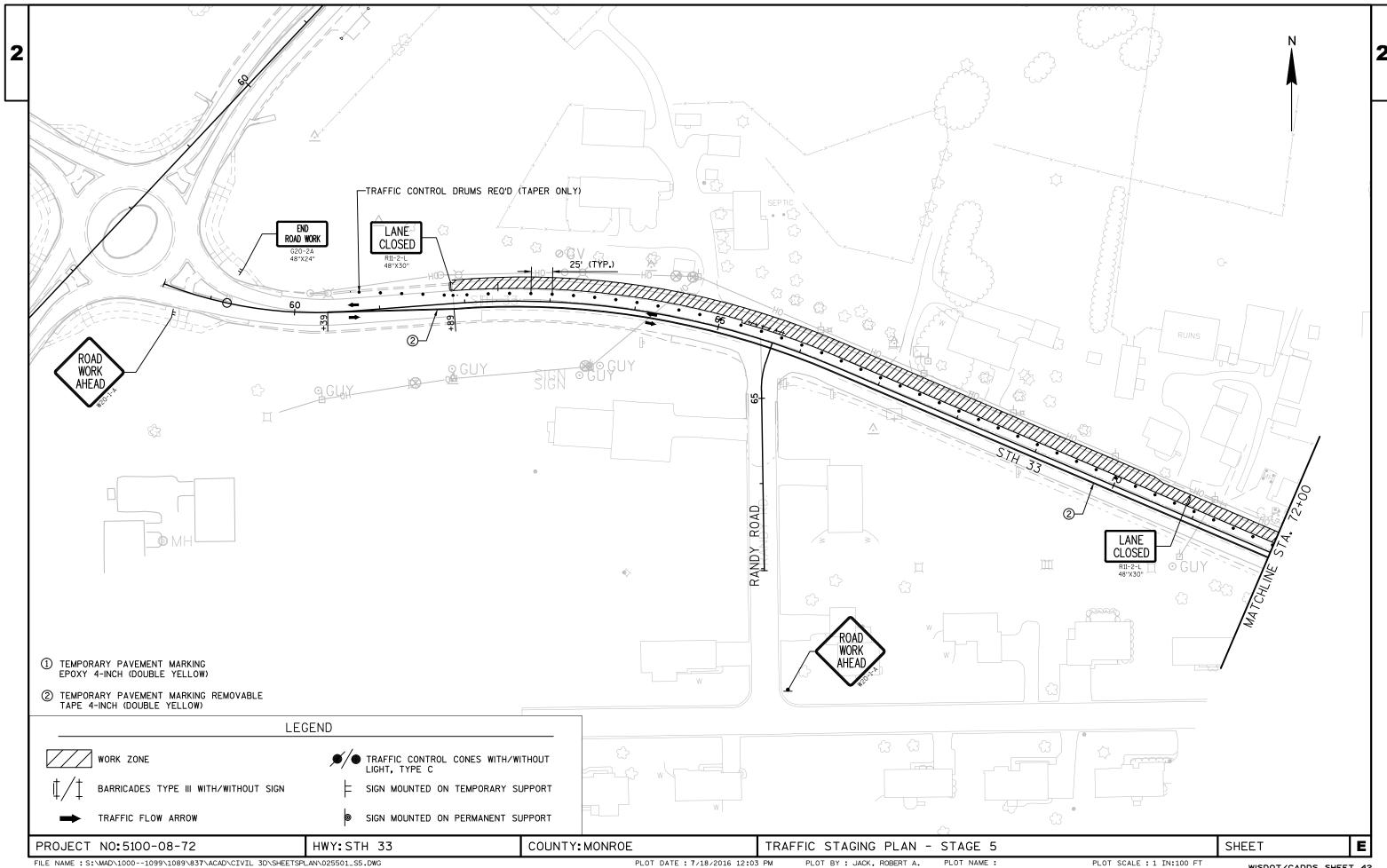


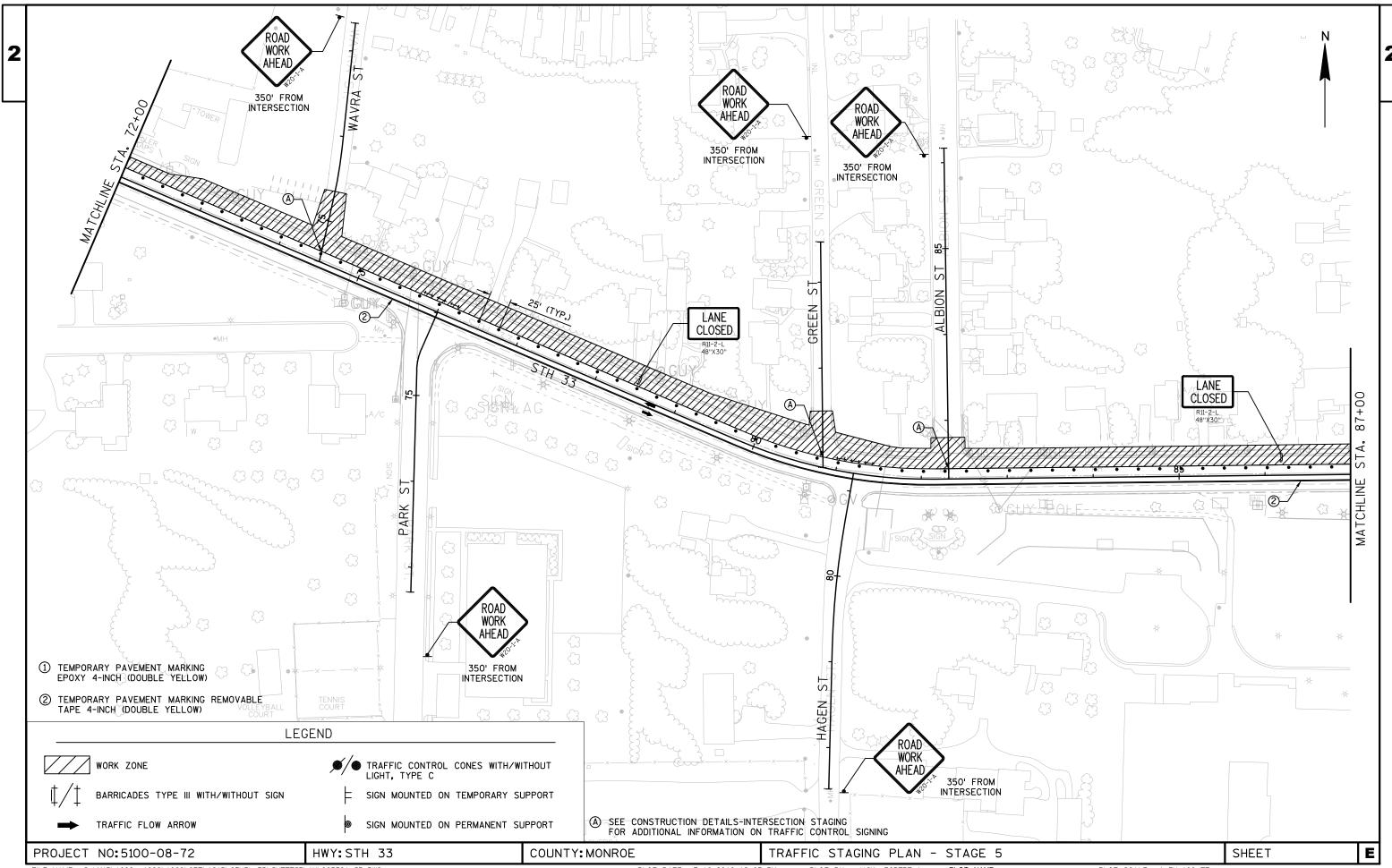


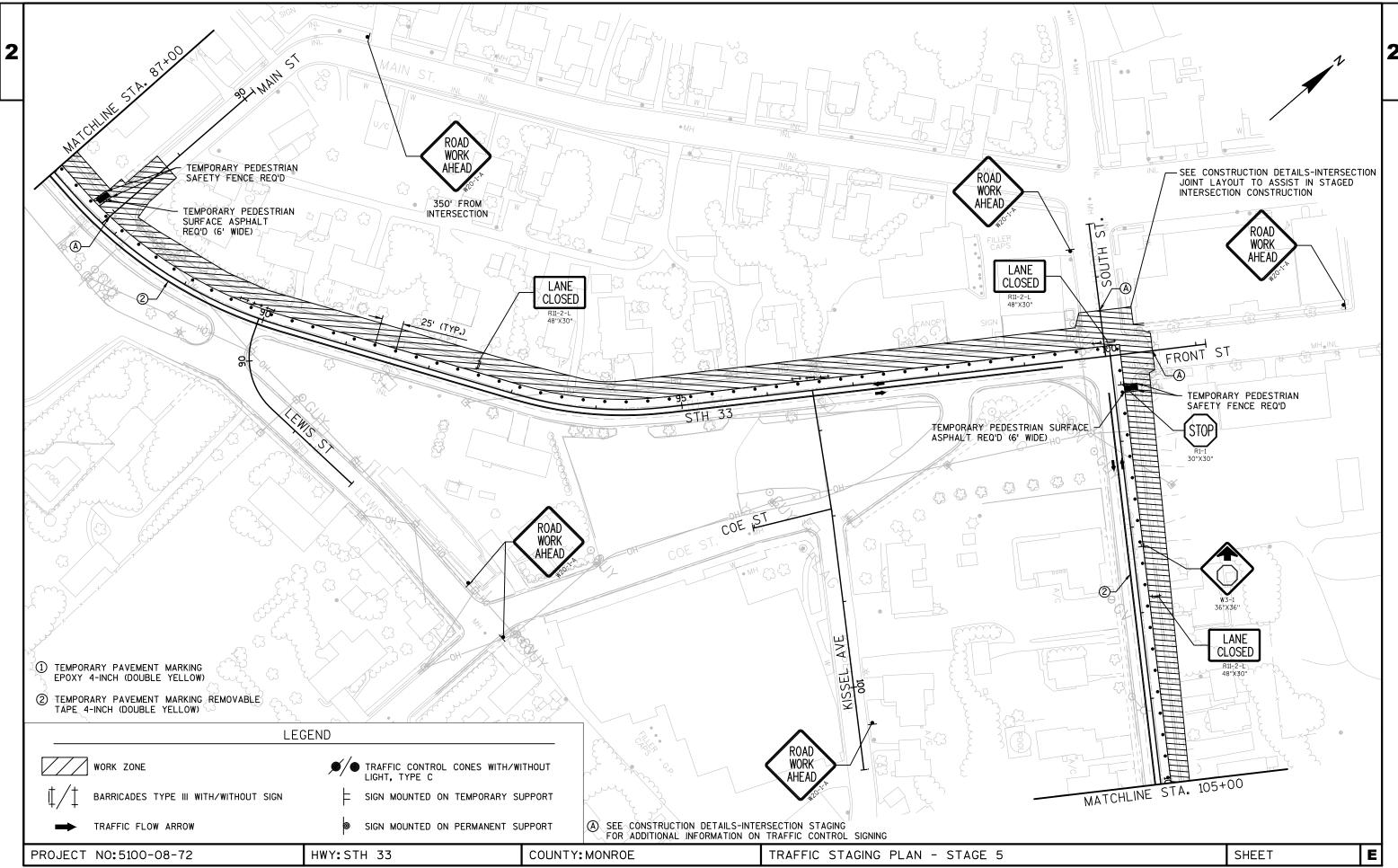


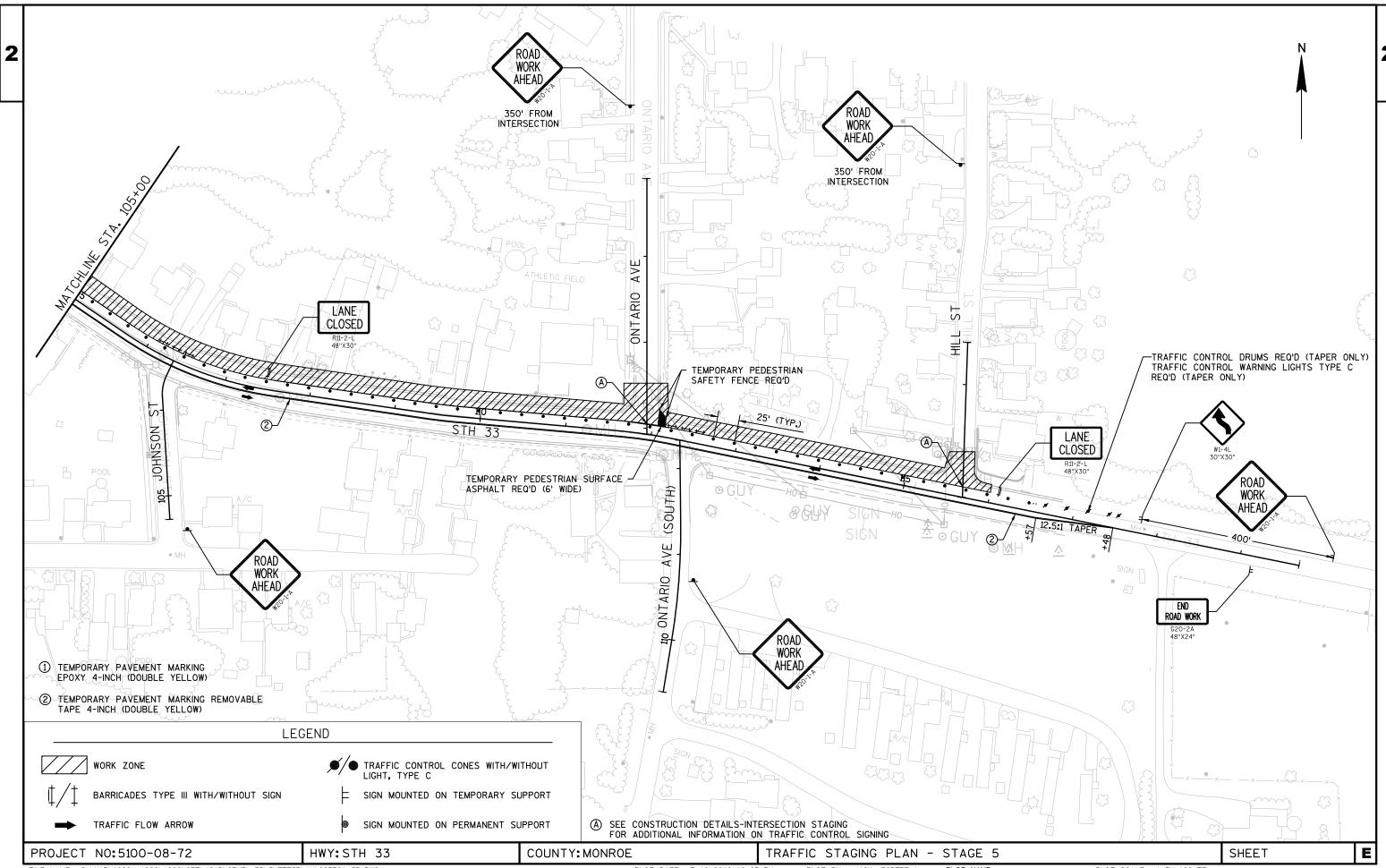


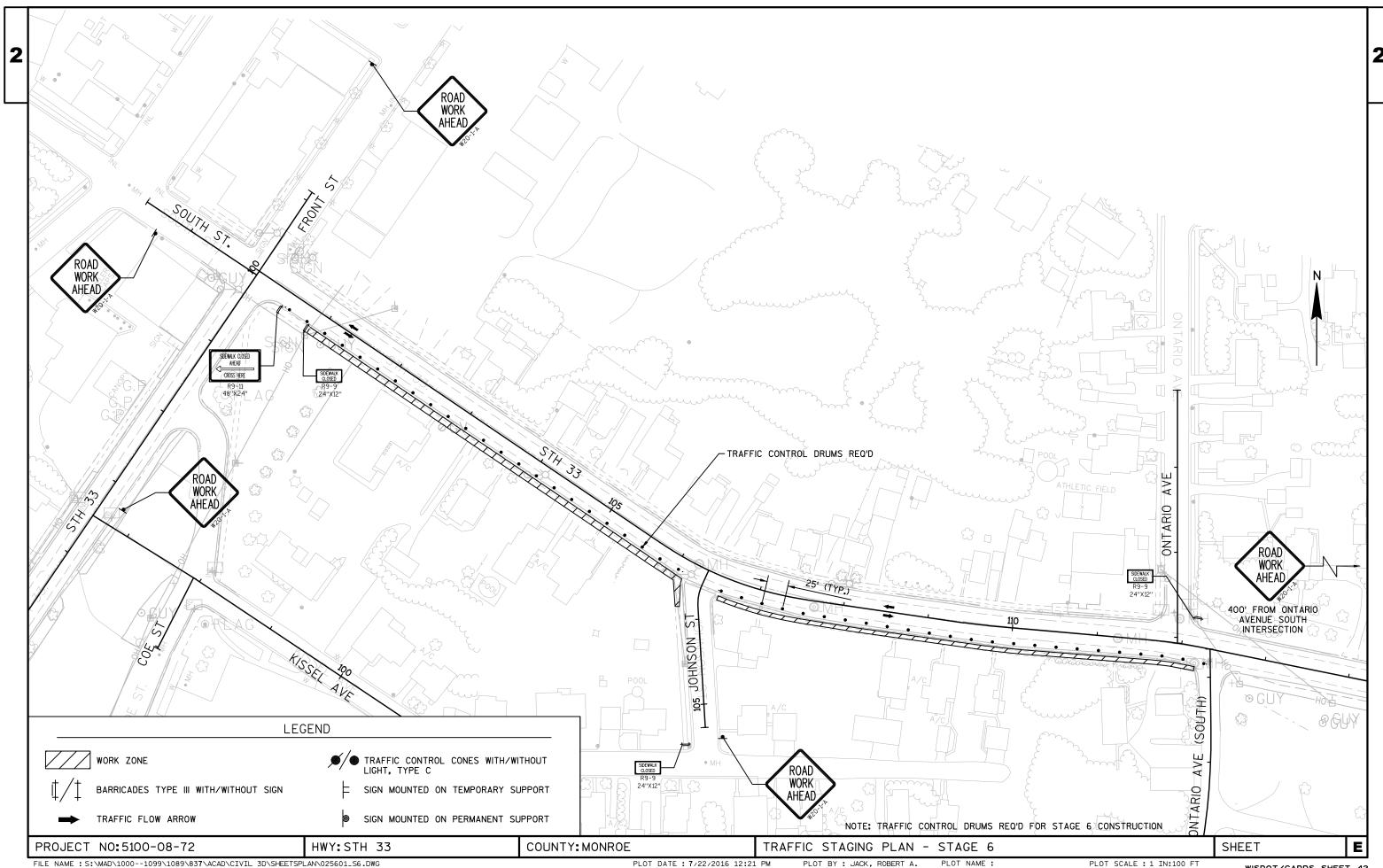


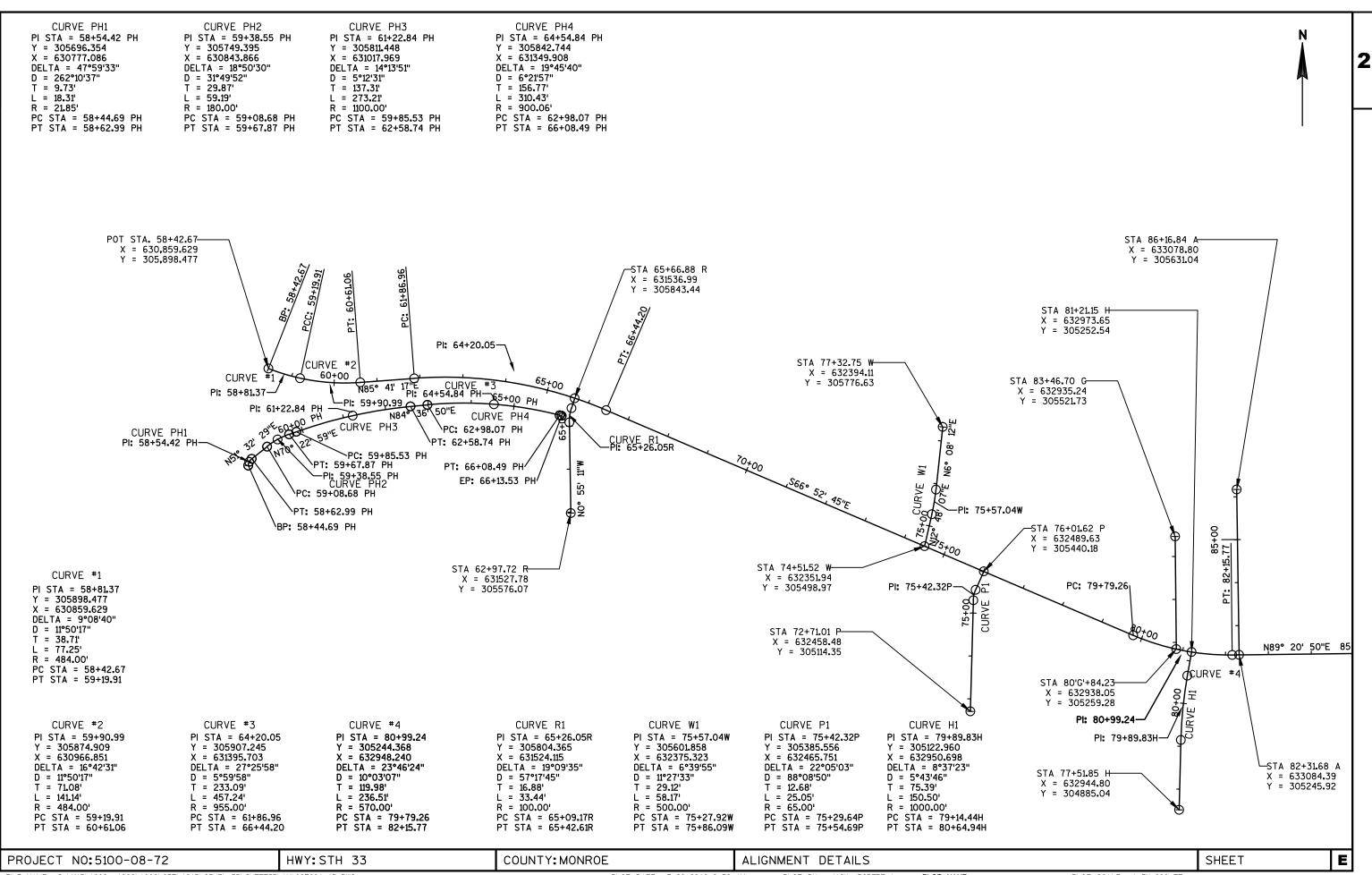


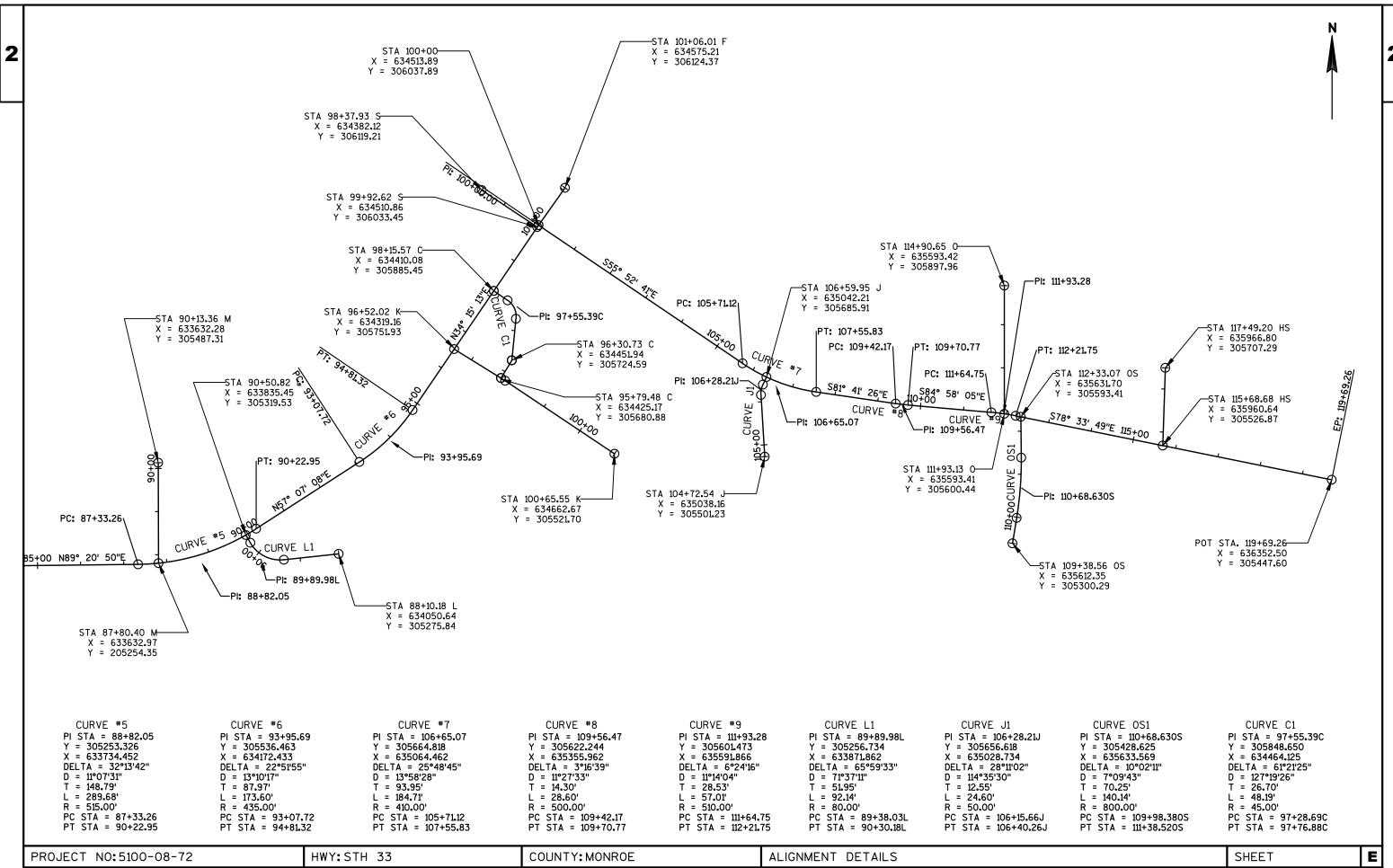


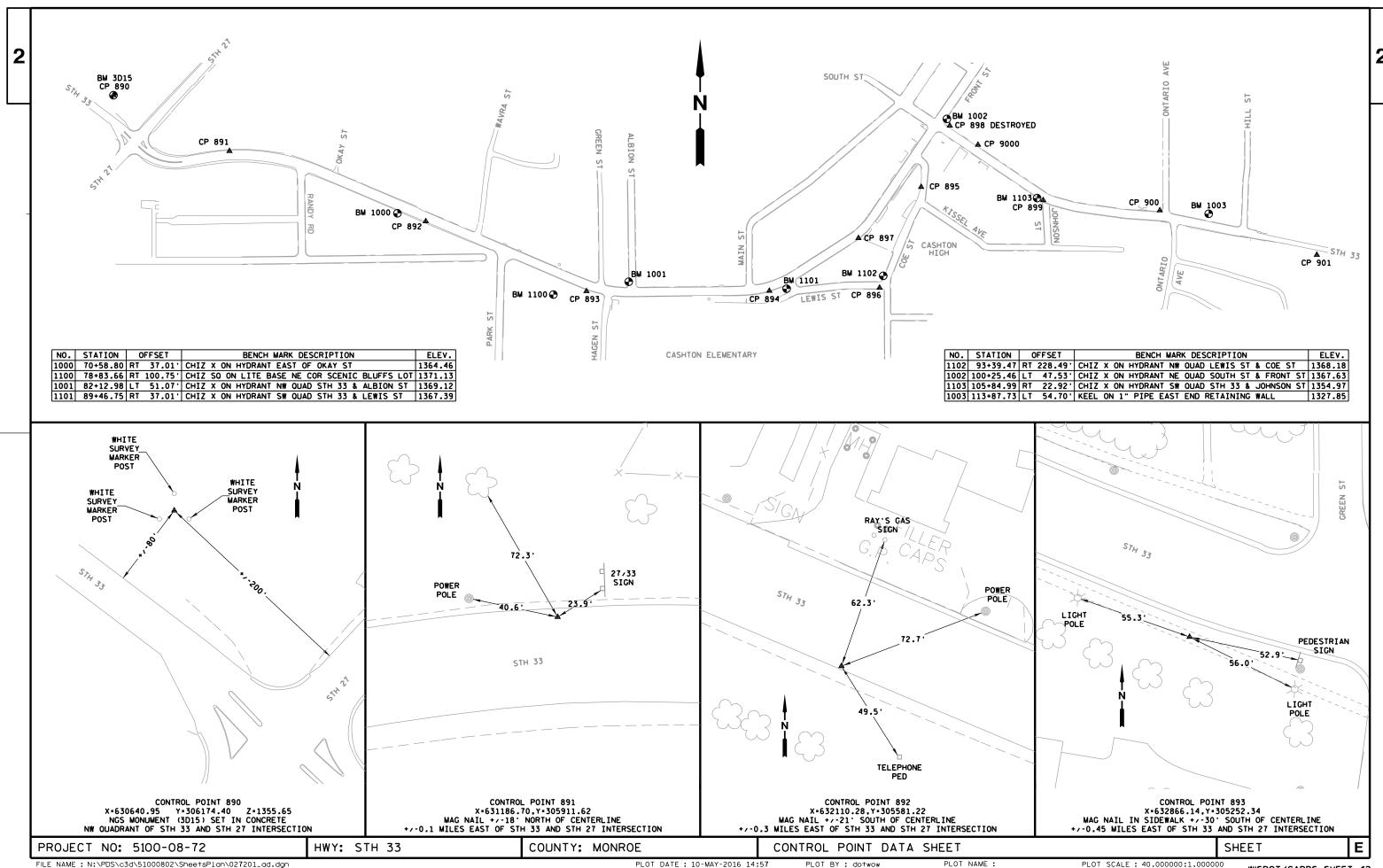


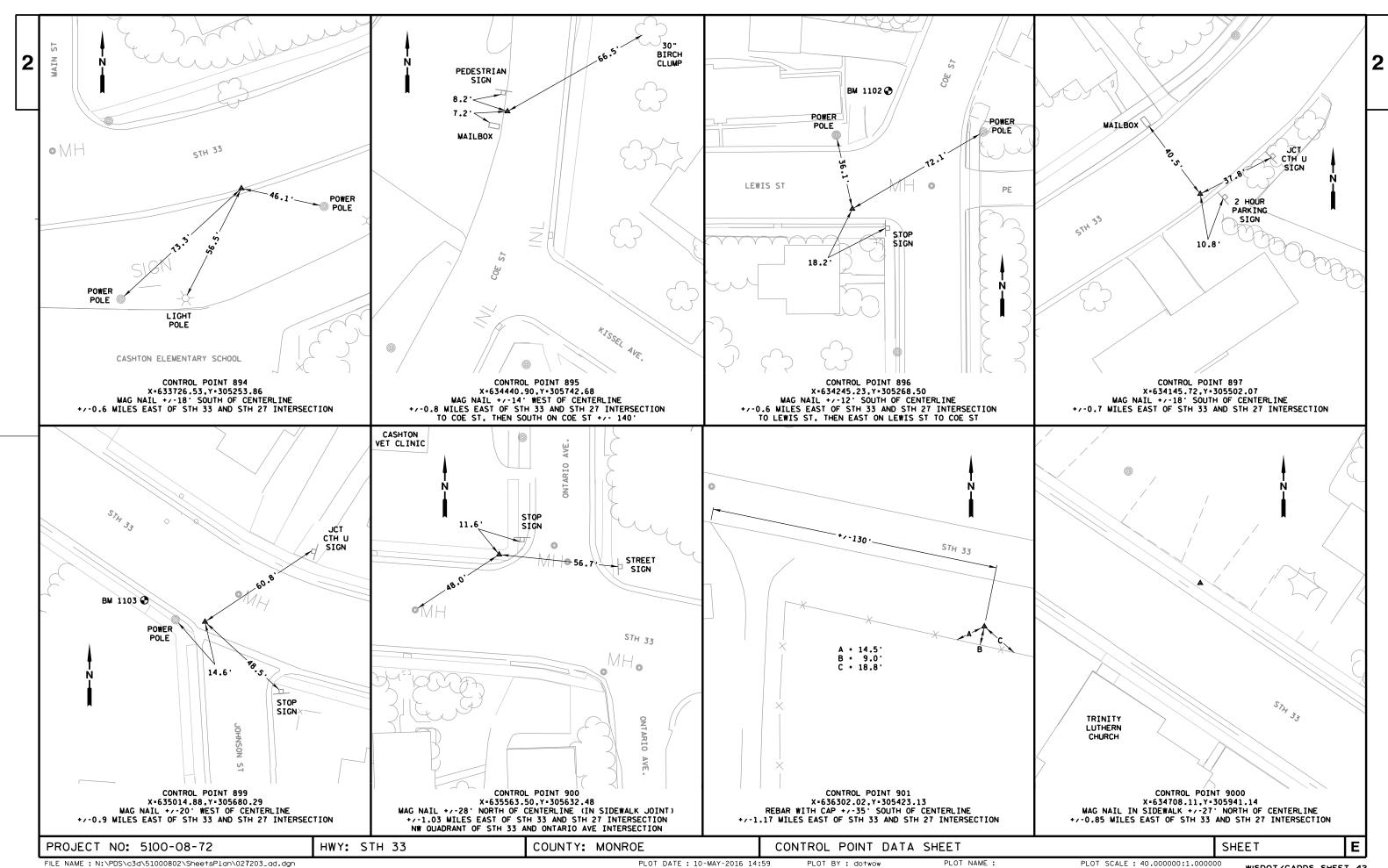












DATE 11	0CT16	E S ⁻	TIMAT	E O F Q U A N	
LINE		LTEM DECODURTION		T07.1	5100-08-72
NUMBER		I TEM_DESCRIPTION	UNIT	TOTAL	QUANTI TY
0010	201. 0120	Clearing	ID	273.000	273. 000
0020	201. 0220	Grubbi ng	ID	303.000	303.000
0030	203. 0100	Removing Small Pipe Culverts	EACH	1.000	1.000
0040	204. 0100	Removing Pavement	SY	740.000	740. 000
0050	204. 0150	Removing Curb & Gutter	LF	8, 950. 000	8, 950. 000
0060	204. 0155	Removing Concrete Sidewalk	SY	3, 100. 000	3, 100. 000
0070	204. 0195	Removing Concrete Bases	EACH	1. 000	1. 000
0800	204. 0210	Removing Manholes	EACH	2.000	2. 000
0090	204. 0220	Removing Inlets	EACH	17. 000	17. 000
0100	204. 0245	Removing Storm Sewer (size) 01. 10-Inch	LF	216. 000	216. 000
0110	204. 0245	Removing Storm Sewer (size) 02. 12-Inch	LF	403.000	403. 000
0120	204. 0245	Removing Storm Sewer (size) 03. 18-Inch	LF	42.000	42.000
0130	204. 0245	Removing Storm Sewer (size) 04. 24-Inch	LF	182. 000	182. 000
0140	204. 0245	Removing Storm Sewer (size) 05. 30-Inch	LF	220.000	220.000
0150	204. 9060. S	Removing (item description) 01. Inlet	EACH	4.000	4. 000
		Covers			
0160	204. 9060. S	Removing (item description) 02.	EACH	1. 000	1. 000
		Commercial Sign			
0170	204. 9165. S	Removing (item description) 01. Brick	SF	24. 000	24. 000
0100	20E 0100	Pavers	CV	20 45/ 000	20 45/ 222
0180	205. 0100	Excavation Common	CY	30, 456. 000	30, 456. 000
0190	205. 0501. S	Excavation, Hauling, and Disposal of	TON	420. 000	420. 000
0200	212 0100	Petroleum Contaminated Soil	EACH	1 000	1 000
0200	213. 0100	Finishing Roadway (project) 01. 5100-08-72	EACH	1. 000	1. 000
		3100-00-72			
0210	214. 0100	Obliterating Old Road	STA	1. 800	1. 800
0220	305. 0110	Base Aggregate Dense 3/4-Inch	TON	184. 000	184. 000
0230	305. 0120	Base Aggregate Dense 1 1/4-Inch	TON	17, 190. 000	17, 190. 000
0240	310. 0115	Base Aggregate Open-Graded	CY	75. 000	75. 000
0250	312. 0110	Select Crushed Material	TON	22, 538. 000	22, 538. 000
0260	405. 0100	Coloring Concrete WisDOT Red	CY	48. 000	48. 000
0270	415. 0080	Concrete Pavement 8-Inch	SY	23, 805. 000	23, 805. 000
0280	415. 0210	Concrete Pavement Gaps	EACH	23. 000	23. 000
0290	415. 1080	Concrete Pavement HES 8-Inch	SY	633. 000	633. 000
0300	416. 0160	Concrete Driveway 6-Inch	SY	1, 340. 000	1, 340. 000
0310	416. 0260	Concrete Driveway HES 6 Inch	SY	85.000	85. 000
	416. 0260 416. 0620	Concrete Driveway HES 6-Inch Drilled Dowel Bars	SY EACH	168. 000	168. 000
0320 0330	416. 0620 440. 4410	Incentive IRI Ride	DOL	4, 052. 000	4, 052. 000
0330	455. 0605	Tack Coat	GAL	4, 052, 000 18, 000	18. 000
0340		Incentive Density HMA Pavement	DOL	70. 000	70. 000
5550	-100. Z000	Theoretive Density IIWIA ravellient	DOL	70.000	70.000
0360	460. 5223	HMA Pavement 3 LT 58-28 S	TON	59.000	59. 000
0370	460. 5224	HMA Pavement 4 LT 58-28 S	TON	39. 000	39. 000
0380	465. 0105	Asphal ti c Surface	TON	645. 000	645. 000
0390	465. 0120	Asphaltic Surface Driveways and Field	TON	129. 000	129. 000
		Entrances			
0400	465. 0125	Asphaltic Surface Temporary	TON	620. 000	620. 000
0410	44E 0210	Applied to Curb	LF	2 425 000	2 425 000
0410	465. 0310 465. 0315	Asphaltic Curb Asphaltic Flumes	SY	3, 425. 000 9. 000	3, 425. 000 9. 000
0420	520. 8000	Concrete Collars for Pipe	SY EACH	9. 000 10. 000	9. 000 10. 000
0430		Apron Endwalls for Culvert Pipe		1. 000	1. 000
0440	522. 1012	Reinforced Concrete 12-Inch	EACH	1.000	1.000
0450	522. 1024	Apron Endwalls for Culvert Pipe	EACH	1. 000	1. 000
0450	JZZ. 1UZ4	Reinforced Concrete 24-Inch	LACT	1.000	1.000
		NOTH OF COURSE OF THE Z4-THCH			
0460	522. 1036	Apron Endwalls for Culvert Pipe	EACH	1. 000	1. 000
3 100	J22. 1000	Reinforced Concrete 36-Inch	2,1011	1.000	1.000

DATE 11 LINE	0CT16		ESTIMATE	OF QUAN	ITITIES 5100-08-72	
NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	QUANTI TY	
0470 0480	601. 0409 601. 0411	Concrete Curb & Gutter 30-Inch Type Concrete Curb & Gutter 30-Inch Type		9, 645. 000 758. 000	9, 645. 000 758. 000	
0490	602. 0410	Concrete Sidewalk 5-Inch	SF	37, 800. 000	37, 800. 000	
0500	602. 0505	Curb Ramp Detectable Warning Field Yellow	SF	604. 000	604. 000	
0510	602. 1500	Concrete Steps	SF	48. 000	48. 000	
0520	606. 0200	Riprap Medium	CY	16. 000	16. 000	
0530	608. 0412	Storm Sewer Pipe Reinforced Concrete		2, 240. 000	2, 240. 000	
0540	608. 0415	Class IV 12-Inch Storm Sewer Pipe Reinforced Concrete Class IV 15-Inch	e LF	278. 000	278. 000	
0550	608. 0418	Storm Sewer Pipe Reinforced Concrete	e LF	4. 000	4. 000	
		Class IV 18-Inch				
0560	608. 0421	Storm Sewer Pipe Reinforced Concrete	e LF	13. 000	13. 000	
0570	608. 0424	Class IV 21-Inch Storm Sewer Pipe Reinforced Concrete	e LF	2, 080. 000	2, 080. 000	
0500	(00.0105	Class IV 24-Inch				
0580	608. 0430	Storm Sewer Pipe Reinforced Concrete Class IV 30-Inch	e LF	6. 000	6. 000	
0590	608. 0436	Storm Sewer Pipe Reinforced Concrete Class IV 36-Inch	e LF	450. 000	450. 000	
0600	610. 0419	Storm Sewer Pipe Reinforced Concrete	e LF	59. 000	59. 000	
		Horizontal Elliptical Class HE-IV 19x30-Inch				
0610	611. 0535	Manhol e Covers Type J-Special	EACH	22. 000	22. 000	
0620	611. 0624	Inlet Covers Type H	EACH	40.000	40.000	
0630 0640	611. 0639 611. 0642	Inlet Covers Type H-S Inlet Covers Type MS	EACH EACH	19. 000 2. 000	19. 000 2. 000	
0650	611. 0645	Inlet Covers Type MS-A	EACH	2. 000	2. 000	
0660	611. 2004	Manholes 4-FT Diameter	EACH	7. 000	7. 000	
0670	611. 2005	Manholes 5-FT Diameter	EACH	15. 000	15. 000	
0680	611. 2006	Manholes 6-FT Diameter	EACH	2.000	2. 000	
0690	611. 3004	Inlets 4-FT Diameter	EACH	20.000	20.000	
0700	611. 3230	Inlets 2x3-FT	EACH	38. 000	38. 000	
0710	611. 3901	Inlets Median 1 Grate	EACH	4. 000	4. 000	
0720	611. 8110	Adjusting Manhole Covers S Cover Plates Temporary	EACH	14. 000	14. 000	
0730 0740	611. 8120. 3	S Cover Plates Temporary Pipe Underdrain 6-Inch	EACH LF	2. 000 2, 610. 000	2. 000 2, 610. 000	
0750		S Insulation Board Polystyrene (inch)		36. 000	36. 000	
		2-I nch				
0760	619. 1000	Mobilization	EACH	1.000	1.000	
0770 0780	624. 0100 625. 0100	Water Topsoi I	MGAL SY	324. 000 14, 344. 000	324. 000 14, 344. 000	
0780	627. 0200	Mul chi ng	SY	8, 129. 000	8, 129. 000	
0800	628. 1504	Silt Fence	LF	3, 550. 000	3, 550. 000	
0810	628. 1520	Silt Fence Maintenance	LF	6, 475. 000	6, 475. 000	
0820	628. 1905	Mobilizations Erosion Control	EACH	15. 000	15. 000	
0830	628. 1910 628. 2006	Mobilizations Emergency Erosion Cont Erosion Mat Urban Class I Type A		8. 000 7, 070. 000	8. 000 7, 070. 000	
0840 0850	628. 7005	Inlet Protection Type A	SY EACH	69. 000	69. 000	
0860	628. 7010	Inlet Protection Type B	EACH	5. 000	5. 000	
0870	628. 7015	Inlet Protection Type C	EACH	63.000	63.000	
0880						
	J20. 7370	Nock bags	LACII	20.000		
0880 0890 0900	628. 7020 628. 7504 628. 7570	Inlet Protection Type D Temporary Ditch Checks Rock Bags	EACH LF EACH	6. 000 161. 000 28. 000	6. 000 161. 000 28. 000	

DATE 11	0CT16	EST	I M A	ΓΕ Ο F QUAN		
LI NE NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	5100-08-72 QUANTI TY	
0910	629. 0210	Fertilizer Type B	CWT	19. 000	19. 000	
0920 0930	630. 0110 630. 0130	Seeding Mixture No. 10 Seeding Mixture No. 30	LB LB	200. 000 141. 000	200. 000 141. 000	
0930 0940	630. 0140	Seeding Mixture No. 30	LB	118. 000	118. 000	
0950	630. 0200	Seeding Temporary	LB	42. 000	42. 000	
960 970	634. 0614 634. 0616	Posts Wood 4x6-Inch X 14-FT Posts Wood 4x6-Inch X 16-FT	EACH EACH	34. 000 13. 000	34. 000 13. 000	
0970	634. 0618	Posts Wood 4x6-Inch X 18-FT	EACH	10. 000	10. 000	
0990	637. 2210	Signs Type II Reflective H	SF	259. 920	259. 920	
1000	637. 2230	Signs Type II Reflective F	SF	214. 000	214. 000	
1010	638. 2602 638. 3000	Removing Signs Type II Removing Small Sign Supports	EACH EACH	56. 000 56. 000	56. 000 56. 000	
1020 1030	642. 5201	Field Office Type C	EACH	1. 000	1. 000	
1040	643. 0100	Traffic Control (project) 01. 5100-08-72	EACH	1. 000	1. 000	
1050	643. 0300	Traffic Control Drums	DAY	4, 554. 000	4, 554. 000	
1060	643. 0420	Traffic Control Barricades Type III	DAY	8, 064. 000	8, 064. 000	
1070 1080	643. 0715 643. 0900	Traffic Control Warning Lights Type C Traffic Control Signs	DAY DAY	816. 000 6, 932. 000	816. 000 6, 932. 000	
1090		Traffic Control Signs PCMS	DAY	32. 000	32. 000	
1100	644. 1410. S	Temporary Pedestrian Surface Asphalt	SF	2, 556. 000	2, 556. 000	
1110		Temporary Pedestrian Surface Plywood	SF	250. 000	250. 000	
1120 1130		Temporary Pedestrian Surface Plate Temporary Curb Ramp	SF EACH	250. 000 4. 000	250. 000 4. 000	
1140		Temporary Pedestrian Safety Fence	LF	403. 000	403. 000	
1150		Geotextile Type DF Schedule A	SY	1, 470. 000	1, 470. 000	
1160	645. 0120	Geotextile Type HR	SY	53.000	53.000	
1170 1180	645. 0220 646. 0106	Geogrid Type SR Pavement Marking Epoxy 4-Inch	SY LF	31, 120. 000 20, 091. 000	31, 120. 000 20, 091. 000	
1190	646. 0600	Removing Pavement Markings	LF	14, 105. 000	14, 105. 000	
1200	647. 0556	Pavement Marking Stop Line Epoxy 12-Inch	LF	34. 000	34. 000	
1210	647. 0766	Pavement Marking Crosswalk Epoxy 6-Inch	LF	640. 000	640. 000	
1220	649. 0400	Temporary Pavement Marking Removable Tape 4-Inch	LF	13, 290. 000	13, 290. 000	
1230	649. 0403	Temporary Pavement Marking Epoxy 4-Inch	LF	21, 410. 000	21, 410. 000	
1240	650. 4000	Construction Staking Storm Sewer	EACH	86.000	86. 000	
1250 	650. 4500	Construction Staking Subgrade	LF 	6, 231. 000	6, 231. 000	
1260	650. 5000	Construction Staking Base	LF	528. 000	528. 000	
1270	650. 5500	Construction Staking Curb Gutter and Curb & Gutter	LF	758. 000	758. 000	
1280	650. 7000	Construction Staking Concrete Pavement	LF	5, 703. 000	5, 703. 000	
1290	650. 8500	Construction Staking Electrical	LS	1. 000	1. 000	
1300	650. 9910	Installations (project) 01. 5100-08-72 Construction Staking Supplemental	LS	1. 000	1. 000	
.000	550. 7710	Control (project) 01. 5100-08-72	LU	1. 000	1.000	
1310	650. 9920	Construction Staking Slope Stakes	LF	6, 521. 000	6, 521. 000	
1320	652. 0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	7, 542. 000	7, 542. 000	
1330	654. 0101	Concrete Bases Type 1	EACH	3.000	3. 000	
1340	654. 0105 654. 0224	Concrete Bases Type 5	EACH	28. 000	28. 000	
1350	654. 0224	Concrete Control Cabinet Bases Type L24	EACH	2. 000	2. 000	
1360	655. 0610	Electrical Wire Lighting 12 AWG	LF	4, 200. 000	4, 200. 000	
1370 1380	655. 0615 655. 0620	Electrical Wire Lighting 10 AWG Electrical Wire Lighting 8 AWG	LF LF	7, 095. 000 23, 004. 000	7, 095. 000 23, 004. 000	
1390	656. 0200	Electrical Service Meter Breaker	LS	1. 000	1. 000	
		Pedestal (Location) 01. East Cabinet				

DATE 11 LINE	0CT16	EST	IMAT	E OF QUAN	T I T I E S 5100-08-72
NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	QUANTI TY
1400	656. 0200	Electrical Service Meter Breaker Pedestal (location) 02. West Cabinet	LS	1. 000	1. 000
1410	657. 0100	Pedestal Bases	EACH	3. 000	3. 000
1420	657. 0255	Transformer Bases Breakaway 11 1/2-Inch Bolt Circle	EACH	28. 000	28. 000
1430	657. 0322	Poles Type 5-Aluminum	EACH	28. 000	28. 000
1440	657. 0420	Traffic Signal Standards Aluminum 13-FT	EACH	3.000	3.000
1450	657. 0710	Luminaire Ārms Truss Type 4 1/2-Inch Clamp 12-FT	EACH	28. 000	28. 000
1460	659. 1120	Luminaires Utility LED B	EACH	28. 000	28. 000
1470	659. 2124	Lighting Control Ćabinets 120/240 24-Inch	EACH	2. 000	2. 000
1480	690. 0150	Sawing Asphalt	LF	18, 080. 000	18, 080. 000
1490	690. 0250	Sawing Concrete	LF	1, 484. 000	1, 484. 000
1500	715. 0415	Incentive Strength Concrete Pavement	DOL	7, 350. 000	7, 350. 000
1510	ASP. 1TOA	On-the-Job Training Apprentice at \$5.	HRS	1, 600. 000	1, 600. 000
1520	ASP. 1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	990.000	990.000
1530	SPV. 0045	Special 01. Traffic Control Cones	DAY	23, 092. 000	23, 092. 000
1540	SPV. 0060	Special O1. Pull Boxes Non-Conductive 24x42-Inch	EACH	30.000	30. 000
1550	SPV. 0060	Special 02. Rapid Rectangular Flashing Beacon	EACH	4. 000	4. 000
1560	SPV. 0060	Special 03. Inlet Covers Type DW	EACH	5. 000	5. 000
1570	SPV. 0060	Special 04. Storm Sewer Tap	EACH	1. 000	1. 000
1580	SPV. 0060	Special O5. Adjust Water Valve	EACH	54.000	54.000
1590	SPV. 0060	Special O6. Construction Staking Curb Ramp	EACH	52. 000	52. 000
1600	SPV. 0090	Special 01. Concrete Curb & Gutter HES 30-Inch Type A	LF	60. 000	60. 000
1610	SPV. 0105	Special 01. Concrete Pavement Joint Layout	LS	1. 000	1. 000
1620	SPV. 0120	Special 01. Water for Seeded Areas	MGAL	318.000	318. 000
1630	SPV. 0180	Special O1. Temporary Surface Same Day	SY	601.000	601. 000
1640	SPV. 0180	Special 02. Concrete Pavement SHES 8-Inch	SY	62. 000	62. 000

204.0220

REMOVING

INLETS

EACH

1 1

1

1

1 1

1 1

17

	CLEARING A	ND GRUBBIN	G SUMMARY			REMOVING CURB & GU	ITTER			REMOVING	STORM SEWE	ER STRUCTURES	<u>; </u>
CATEGORY	STATION	LOCATION		201.0220 GRUBBING ID	<u>CATEGORY</u> 0010	STATION - STATION 58+65.72 - 81+20.85	LOCATION RT/LT	204.0150 LF 1,650				204.0210 REMOVING MANHOLES	2 R
					0010	81+20.85 - 96+52.46	RT/LT	3,400	CATEGORY	STATION	OFFSET	EACH	
0010	82+86	RT		30		96+52.46 - 106+35.08	RT/LT	2,000	0010	67.00	221		
	88+78	LT	26	26		106+35.08 - 116+00.31	RT/LT	1,600	0010	67+33	29' RT		
	88+80	LT	5	5		96+97.20 K - 98+42.54 K	RT/LT	300		67+33	50' LT		
	89+09	LT	24	24						85+48	37' RT		
-	89+21	LT	22	22			TOTAL	8,950		85+91	35' RT		
	90+88	RT	22	22						87+65	20' RT		
	91+46	RT	22	22						88+36 M	14' LT		
	91+86	RT	21	21						91+56	22' LT		
	92+21	RT	23	23						91+56	18' RT		
	92+86	RT	12	12						96+55	22' LT		
	93+78	RT	14	14		REMOVING CONCRETE SI	DEWALK		-	96+62	19' RT		
	93+92	RT	13	13				<u> </u>		98+08 K	19' LT		
	94+11	RT	11	11				204.0155		98+13 K	30' RT		
	94+81	RT	23	23	CATEGORY	STATION - STATION	LOCATION	SY		110+54	22' LT		
	95+29	RT	10	10						112+22	26' RT		
	96+25	RT	25	25	0010	58+65.72 - 81+20.85	RT/LT	640	-	112+35	15' RT	1	
						81+20.85 - 96+52.46	RT/LT	880		112+26 0	18' LT		
		TOTALS	273	303		96+52.46 - 106+35.08	RT/LT	920		112+29 0	4' LT	1	
						106+35.08 - 116+00.31	RT/LT	620		112+32 0	14' RT		
						96+97.20 K - 98+42.54 K	RT/LT	40		115+35	20' LT		
	REMOV	ING SMALL I	PIPE CULVE	RTS			TOTAL	3,100			TOTALS	2	_

0010 58+65.72 - 81+20.85 LT 170 81+20.85 - 96+52.46 LT 140 96+52.46 - 106+35.08 RT/LT 310
96+52.46 - 106+35.08 RT/LT 310
106+35.08 - 116+00.31 LT 120

204.0195

EACH

1

EACH

1

REMARKS

18" RCCP - 131'

					204.0245		
		-	.01	.02	.03	.04	.05
			10-INCH	12-INCH	18-INCH	24-INCH	30-INC
CATEGORY	STATION - STATION	LOCATION	LF	LF	LF	LF	LF
0010	67+33	LT/RT				80	
	85+48 - 85+91	RT	43				
	85+91 - 87+64	RT	173				
_	87+65	LT/RT		100			
	91+56	LT/RT			42		
	96+58	LT/RT				42	
	98+10 K	LT/RT		48			
	96+73 к - 98+08 к	LT		138			
	110+55 - 112+67	LT/RT					220
_	112+20 - 112+35	RT		15			
_	112+26 0 - 112+33 0	LT/RT		32			
	111+87 - 112+34	LT/RT		70			
	115+35	LT/RT				60	
	_	TOTALS	216	403	42	182	220

PROJECT NO:5100-08-72	HWY:STH 33	COUNTY: MONROE	MISCELLANEOUS QUANTITIES		SHEET	E
FILE NAME : S:\MAD\10001099\1089\837\ACAD\CIVIL 3D\SHEETSPL	AN\030201_MQ.DWG	PLOT DATE : 8/26/2016 9:00	AM PLOT BY: JACK, ROBERT A. PLOT NAME:	PLOT SCALE : #########	WISDOT/CADDS SHEE	

LOCATION

RT

STATION

97+70

CATEGORY STATION OFFSET

76+24 39' RT

0010

CATEGORY

0010

EARTHWORK

			EXCAV	205.0100 ATION COMMON (1)	AVAILABLE	UNEXPANDED	EXPANDED	MASS ORDINATE		* 312.0110 SELECT CRUSHED
		_	CUT	EBS EXCAVATION (2)	MATERIAL (3)	FILL	FILL (4)	+/- (5)	WASTE	MATERIAL (6)
							FACTOR			
				5% OF CUT			1.25			
CATEGORY	LOCATION	STATION - STATION	CY	CY	CY	CY	CY		CY	TON
0010	STH 33 STAGE 1	60+35 - 115+43	1,231	62	1,231	58	73	1,158	1,158	120
0010	STH 33	60+35 - 116+00	23,471	1,174	23,471	731	914	22,557	22,557	2,250
	RANDY ROAD	64+85 R - 65+49 R	187	9	187	12	15	172	172	20
	WAVRA STREET	74+74 W - 75+35 W	276	14	276	1	1	275	275	25
	PARK STREET	75+22 P - 75+84 P	240	12	240	0	0	240	240	25
_	GREEN STREET	81+07 G - 81+50 G	120	6	120	0	0	120	120	10
	HAGEN STREET	80+75 H - 81+03 H	109	5	109	0	0	109	109	10
	ALBION STREET	82+54 A - 82+80 A	103	5	103	0	0	103	103	10
	MAIN STREET	88+03 M - 88+80 M	185	9	185	0	0	185	185	20
	LEWIS STREET	89+32 L - 90+33 L	583	29	583	0	0	583	583	55
_	KISSEL AVENUE	96+70 к - 98+43 к	734	37	734	0	0	734	734	70
	JOHNSON STREET	106+06 J - 106+42 J	127	6	127	0	0	126	126	10
	ONTARIO AVENUE	112+11 O - 112+45 O	98	5	98	1	1	97	97	9
	ONTARIO AVENUE	111+81 os - 112+15 os	96	5	96	0	1	95	95	9
	HILL STREET	115+87 н - 116+90 н	111	6	111	0	0	111	111	10
_	MULTI-USE PATH	58+45 PH - 66+14 PH	579	29	579	41	52	527	527	55
	DRIVEWAYS	61+87 - 116+00	754	38	754	0	0	754	754	70
		SUBTOTALS	29,006	1,450	29,006	846	1,057	27,948	27,948	2,778
		TOTALS		30,456					=	2,778

*ADDITIONAL QUANTITIES LISTED ELSEWHERE

NOTES:

- 1) EXCAVATION COMMON IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100.
- 2) EBS EXCAVATION TO BE BACKFILLED WITH SELECT CRUSHED MATERIAL.
- 3) AVAILABLE MATERIAL = CUT
- 4) EXPANDED FILL = UNEXPANDED FILL * EXPANDED FILL FACTOR. EXPANDED FILL FACTOR = 1.25.
- 5) MASS ORDINATE: MASS ORDINATE = CUT (FILL * FILL FACTOR)

PLUS MASS ORDINATE QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS MASS ORDINATE QUANTITY INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

6) USED FOR BACKFILL OF EBS

		PROJECT NO:5100-08-72	HWY:STH 33	COUNTY: MONROE		SHEET	E
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		REMOVING	INLET COVE	ERS					BASE	AGGREGATE [ENSE			
		204.9060	.s.01								305.0110	305.0120	* 312.0110 SELECT	645.0220
CATEGORY	LOCATION	EACH	1	REM	IARKS							EGATE DENSE	CRUSHED	GEOGRID
0010	91+56 L				ED STRUCTURE 1		CATEGORY	STAGE	STATION - STATION	LOCATION	3/4-INCH TON	1 1/4-INCH TON	MATERIAL TON	TYPE SR SY
	96+55 L 110+55 L				ED STRUCTURE 1 TING STRUCTURE		0010	STAGE 1	59+00.00 - 81+20.85	LT	30	670		
	115+36 L	т 1	S	TAGE 1 EXIS	TING STRUCTURE	Ē			81+20.85 - 96+52.46 96+52.46 - 106+35.08	LT LT/RT		460 390		
	TOTAL	4							106+35.08 - 116+00.31 SUBTOTALS (STAGE 1)	<u>LT</u>	30	370 1,890	0	0
								STAGE 3	59+00.00 - 81+20.85	RT	25	2,050	3,400	5,350
									81+20.85 - 96+52.46 96+52.46 - 106+35.08	RT RT		1,900 900	2,700 1,600	4,250 2,550
	REMOVING CO	MMERCIAL SIG	SN						106+35.08 - 116+00.31 96+97.20 K - 98+42.54 K	RT LT/RT	25 	950 420	1,550 430	2,450 680
CATEGORY	LOCATI		4.9060.s.02 EACH						SUBTOTALS (STAGE 3)	- · , · · ·	50	6,220	9,680	15,280
								STAGE 4	59+00.00 - 81+20.85 81+20.85 - 96+52.46	LT LT		900 680	1,700 1,300	2,700 2,050
0010	100+34	36' LT	1						96+52.46 - 106+35.08	LT		440	840 810	1,300 1,300
									106+35.08 - 116+00.31 SUBTOTALS (STAGE 4)	LT	0	450 2,470	4,650	7,350
	D E MOV	TNC BRICK DA	WERE					STAGE 5	59+00.00 - 81+20.85	LT	85	1,200	1,450	2,250
	KEMUV	'ING BRICK PA							81+20.85 - 96+52.46 96+52.46 - 106+35.08	LT LT	9 8	950 790	1,150 780	1,800 1,200
CATEGORY	STATION	LOCATIO		.9165.S.01 SF					106+35.08 - 116+00.31 SUBTOTALS (STAGE 5)	LT	2 104	610 3,550	730 4,110	1,150 6,400
0010	89+90 L	LT		24				stage 6	96+52.46 - 106+35.08	RT		120		
									106+35.08 - 116+00.31 SUBTOTALS (STAGE 6)	RT	0	95 215	0	0
									UNDISTRIBUTED			1435		
	OBLIT	TERATING OLD	ROAD		-				SUBTOTALS (0010)		184	15,780	18,440	29,030
CATEGORY	STATION	- STATION	LOCATION	214.0100 STA	_		0020	STAGE 3	59+00.00 - 81+20.85	RT		400		
0010		- 97+87.57		1.8	-				SUBTOTALS (STAGE 3)		0	400	0	0
			·						SUBTOTALS (0020)		0	400	0	0
							0040	STAGE 3	58+44.69 PH - 66+13.53 PH	LT/RT		310		
COLOR	ING CONCRETE	E WISDOT RED			N, HAULING, AN DLEUM CONTAMIN				SUBTOTALS (STAGE 3)		0	310	0	0
CATEGORY	STATION L		.0100		SEEON CONTAINEN				SUBTOTALS (0040)		0	310	0	0
			CY	CATEGORY	PROJECT	205.0501.s TON	0050	STAGE 5	59+00.00 - 81+20.85 81+20.85 - 96+52.46	LT LT		120 280	220 520	350 830
0040	76+32 85+52		6 6						96+52.46 - 106+35.08	LT		200	380	600
	96+22 99+47	LT/RT	6 7	0010	5100-08-72	420			106+35.08 - 116+00.31 SUBTOTALS (STAGE 5)	LT	0	100 700	200 1,320	310 2,090
	99+95 100+01	ĹT	7						SUBTOTALS (0050)		0	700	1,320	2,090
	100+01		9						TOTALS		184	17190	19760	31120

HWY:STH 33

PROJECT NO:5100-08-72

COUNTY: MONROE

MISCELLANEOUS QUANTITIES

SHEET

		DRILLED DOWL BARS						ASPHALTIC :	ITEMS SUMMARY			
CATEGORY	STAGE	STATION	LOCATION	416.0620 EACH				455.0605	460.5223	460.5224	465.0105	465.0120 ASPHALTIC SURFACE
								TACK	НМА РА	VEMENT	ASPHALTIC	DRIVEWAYS AND
0010	STAGE 3	76+30	RT	24				COAT	3 LT 58-28 S	4 LT 58-28 S	SURFACE	FIELD ENTRANCES
		85+50	RT	24	CATEGORY	STATION - STATION	LOCATION	GAL	TON	TON	TON	TON
		96+20	RT	24								
		SUBTOTAL (STAGE 3)		72	0010	59+00.00 - 81+20.85	LT/RT				175	5
						81+20.85 - 96+52.46	LT/RT				84	65
	STAGE 4	76+30	MEDIAN	16		96+52.46 - 106+35.08	LT/RT				12	57
		85+50	MEDIAN	16		106+35.08 - 116+00.31	LT/RT	18	59	39	37	2
		96+20	MEDIAN	16		96+97.20 K - 98+42.54 K	LT/RT				125	
		SUBTOTAL (STAGE 4)		48		SUBTOTALS (0010)		18	59	39	433	129
	STAGE 5	76+30	LT	16	0020	59+00.00 - 81+20.85	RT				120	
		85+50	LT	16		SUBTOTALS (0020)		0	0	0	120	0
		96+20	LT	16								
		SUBTOTAL (STAGE 5)		48	0040	58+44.69 PH - 66+13.53 PH	LT/RT				92	
						SUBTOTALS (0040)		0	0	0	92	0
			TOTAL	168								
							TOTALS	18	59	39	645	129

NOTE: A UNIT WEIGHT OF 110 POUNDS PER SQUARE YARD PER INCH OF THICKNESS WAS USED TO ESTIMATE ASPHALT QUANTITIES.

			CONCR	ETE PAVEME	NT				C0	NCRETE PAVEMENT GAP	S	
CATEGORY	STAGE	STATION -	- STATTON	LOCATION	415.0080 8-INCH SY	415.1080 HES 8-INCH SY	SPV.0180.02 SHES 8-INCH SY	CATEGORY	STAGE	STATION	LOCATION	415.021 EACH
CATEGORY	JIAGE	SIAIION	STATION	LOCATION		<u></u>		0010	STAGE 3	65+65	RT	1
0010	STAGE 3	59+00.00 -	81+20.85	RT	4,160			••		75+65	RT	1
			96+52.46	RT	3,450					81+20	RT	1
		96+52.46 -	- 106+35.08	RT	2,100		62			89+93	RT	1
		106+35.08 -		RT	1,850					106+32	RT	1
	-	SUBTOTAL			11,560	0	62			112+30	RT	1
					,					SUBTOTAL (STAGE 3)		6
	STAGE 4	59+00.00 -	81+20.85	LT	2,700							
		81+20.85 -	96+52.46	LT	2,050				STAGE 4	65+65	MEDIAN	1
		96+52.46 -	106+35.08	LT	1,270	53				74+50	MEDIAN	1
		106+35.08 -	- 116+00.31	LT	1,200					75+65	MEDIAN	1
	_	SUBTOTAL	(STAGE 4)		7,220	53	0			80+91	MEDIAN	1
										81+20	MEDIAN	1
	STAGE 5	59+00.00 -	81+20.85	LT	1,140					82+34	MEDIAN	1
		81+20.85 -	96+52.46	LT	1,000					87+80	MEDIAN	1
		96+52.46 -	- 106+35.08	LT	195	580				89+93	MEDIAN	1
	_	106+35.08 -	- 116+00.31	LT	600					96+50	MEDIAN	1
		SUBTOTAL	(STAGE 5)		2,935	580	0			106+32	MEDIAN	1
										111+88	MEDIAN	1
			SUBTOTAL	s (0010)	21,715	633	62			112+30	MEDIAN	1
										SUBTOTAL (STAGE 4)		12
0050	STAGE 5	59+00.00 -	81+20.85	LT	355							
		81+20.85 -	96+52.46	LT	825				STAGE 5	74+50	LT	1
		96+52.46 -	- 106+35.08	LT	600					80+91	LT	1
	_	106+35.08 -	- 116+00.31	LT	310					82+34	LT	1
	_	SUBTOTAL	(STAGE 5)		2,090	0	0			87+80	LT	1
										111+88	LT	1
			SUBTOTAL	s (0050)	2,090	0	0			SUBTOTAL (STAGE 5)		5
			TOT	ALS	23,805	633	62				TOTAL	23

COUNTY: MONROE

FILE NAME : S:\MAD\1000--1099\1089\837\ACAD\CIVIL 3D\SHEETSPLAN\030201_MQ.DWG LAYOUT NAME - 030201_MQ - 030204_MQ

HWY:STH 33

PROJECT NO:5100-08-72

PLOT DATE : 8/26/2016 9:02 AM

PLOT BY: JACK, ROBERT A. PLOT NAME:

MISCELLANEOUS QUANTITIES

PLOT SCALE : ########

SHEET

	CO	NCRETE DRIV	/EWAY				TE	MPORARY ASPHAL	T SUMMARY					ASPI	HALTIC FLUMES		_
CATEGORY	STATION	LOCATION	416.0160 6-INCH SY	416.0260 HES 6-INCH SY					465.0125 ASPHALTI SURFACE TEMPORAR	ASPHALTIC			CATEGORY		LOCATION	465.0315 SY	_
0010	67+15	LT	10		CATEGORY	STAGE	STATION - STATIO	N LOCATION		Y CURB LF	REMA	RKS	0010	112+73	RT	9	
	68+30	LT	10														
	70+74	LT	10		0010		8+65.72 - 81+20 1+20.85 - 96+52		165 150	700							
	71+58 72+22	LT LT	90 90				1+20.85 - 96+32 6+52.46 - 106+3		150 135	1,280 715							
	72+22	LT	25				06+35.08 - 116+00		125	730							
	73+60	LT	25				UNDISTRIBUTED	LT/RT	45		SIDEROAD CO	ONNECTIONS					
	75+83	LT	20					TOTALS	620	2 425	_						
	76+54	LT	20					TOTALS	620	3,425							
	78+57 78+80	LT LT	20 25														
	83+26	LT	25														
	84+14	LT	15				STO	RM SEWER									
	84+59	LT	15						E22 1012	522.1024 52	2 1026						
	85+29	LT RT	25							WALLS FOR CU							
	85+88 86+48	LT	30 30							NFORCED CONC							
	90+33	LT	20						12-INCH		6-INCH						
	92+42	LT	15		CATEGORY	STRUCTUR	E STATION	LOCATION	EACH		EACH						
	93+19	RT	15														
	93+67 94+40	LT	15		0010	28	114+70.55	33.4' RT			1						
	94+40	RT LT	30 25			31	112+61.12	33.2' RT	1								
	95+74	LT	15			34	115+34.89	35.9' RT		1							
	96+78	LT	75														
	98+50	LT	110					TOTALS	1	1	1						
	99+08 101+13	LT LT	30	85													
	101+13	RT	25														
	102+31	LT	20					CU	RB AND GUT	TER SUMMARY							
	102+73	LT	15														
	102+94	RT	30							601.0409	601.0411	SPV.0090.01		50.5500			
	103+29 103+94	RT LT	10 20							CONCRETE CU	URB & GUTTER	CONCRETE CURE GUTTER HES		STRUCTION TAKING			
	103+54	RT	35								INCH	30-INCH		GUTTER AND			
	104+66	LT	20						_	TYPE A	TYPE D	TYPE A	CURB	& GUTTER			
	104+74	RT	15		CATEGORY	STAGE	STATION -	STATION	LOCATION	LF	LF	LF		LF			
	105+20 105+38	LT RT	35 10		0010	STAGE 3	59+00.00 -	81+20 - 85	RT	1,590	120			120			
	105+38	LT	10		3310	217.GL J	81+20.85 -		RT	1,600	83			83			
	106+44	LT	25				96+52.46 -		RT	925	7			7			
	107+68	LT	25				106+35.08 -		RT	630	7			7			
	108+29	LT	30 45				96+97.20 K - SUBTOTALS		LT/RT	4,745	285 502	0		285 502			
	109+84 110+83	LT LT	45 45				SOBTOTALS	(JIAGE J)		7,773	302	· ·		JUL			
	111+89	RT	25			STAGE 5	59+00.00 -	81+20.85	LT	1,600	49			49			
	113+98	LT	15				81+20.85 -		LT	1,410	140	60		140			
	89+53 L	LT . .	10					106+35.08	LT	1,000 890	9 5.8			9 5 8			
	89+91 L 112+42 O	LT LT	35 5				106+35.08 - SUBTOTALS		LT	4,900	58 256	60		58 256			
	112+42 U	LI	J				SOSTOTALS			.,500							
		TOTALS	1,340	85				=	TOTALS	9,645	758	60		758			
PROJECT N	0:5100-08	8-72		HWY:STH 3	33	I co	OUNTY: MONROE		MISC	ELLANEOUS	QUANTITIES				SHE	ET	E

		CONCRETE SID	EWALK SUMMARY			AD	JUSTING MA	ANHOLE COVE	RS	_
				602.0410 CONCRETE SIDEWALK 5-INCH	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW	CATEGORY0050	STATION 75+00	LOCATION	611.8110 EACH	
CATEGORY	STAGE	STATION - STATION	LOCATION	SF	SF		76+14	18' LT	1	
0010	STACE 2	F0.00 00 81.20 8F	DT	2,800	30		80+98	6' LT	1	
0010	STAGE 3	59+00.00 - 81+20.85 81+20.85 - 96+52.46	RT RT	2,800 8,150	86		82+29	18' LT	1	
		96+52.46 - 106+35.08	RT	1,900	38		84+00	18' LT	1	
		96+97.20 K - 98+42.54 K	LT/RT	1,450	40		85+76	18' LT	1	_
		SUBTOTALS (STAGE 3)	21/101	14,300	194		87+86	15' LT	1	
		(,			101+00	18' LT	1	
	STAGE 5	59+00.00 - 81+20.85	LT	3,950	70		106+13	16' LT	1	
		81+20.85 - 96+52.46	LT	6,850	70		107+66	15' LT	1	
		96+52.46 - 106+35.08	LT	3,200	70		110+46	18' LT	1	_
		106+35.08 - 116+00.31	LT	4,000	40		111+95	18' LT	1	
		SUBTOTALS (STAGE 5)		18,000	250		112+17	14' RT	1	
							115+66	18' LT	1	
	STAGE 6	96+52.46 - 106+35.08	RT	2,350	24					
		106+35.08 - 116+00.31	RT	2,750	30		•	TOTAL	14	_
		SUBTOTALS (STAGE 6)		5,100	54					
		SUBTOTALS (0010)		37,400	498					
0020	STAGE 3	59+00.00 - 81+20.85	RT	300	66					
		SUBTOTALS (STAGE 3)		300	66			COVER I	PLATES TEM	PORARY
		SUBTOTALS (0020)		300	66				8120.s	
0040	STAGE 3	58+44.69 PH - 66+13.53 PH	LT/RT	100	40	CATEGORY	LOCATIO	DN EA	ACH	REMARKS
		SUBTOTALS (STAGE 3)	•	100	40	0010	07.00		1	CT. CT. 1 DD0D0CT
		· · · · · · · · · · · · · · · · · · ·				0010	87+68		1	STAGE 1 PROPOSED STRUCTURE 12
		SUBTOTALS (0040)		100	40		89+50	LT	1	STAGE 1 PROPOSED STRUCTURE 12B
		=	TOTALS	37,800	604		TOTAL		2	

		CONCRETE STEPS					RIPRAP I	ΓEMS	
			602.1500)				606.0200	645.0120
CATEGORY	STATION	LOCATION	SF	REMARKS				RIPRAP	GEOTEXTILE FABRIC
								MEDIUM	TYPE HR
0010	92+21	LT	12	2 RISERS	CATEGORY	STATION	LOCATION	CY	SY
	96+36	LT	12	2 RISERS					
	103+69	RT	12	2 RISERS	0010	112+69	33 'RT	2	8
	107+07	RT	12	2 RISERS		112+82	33 'RT	3	10
	201101					114+81	42 'RT	7	20
	-	TOTAL	48			115+36	48 'RT	4	15
							TOTALS	16	53

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE MISCELLANEOUS QUANTITIES SHEET **E**

			STORM	SEWER PI	PE REINFO	RCED CONCR	RETE							STORM S	EWER PIPE	REINFORCE	D CONCRETI	E CON'T			
							608.0424			610.0419 HORIZONTAL ELLIPTICAL						5 608.0418					HORIZONTA ELLIPTICA
	FROM	ТО	12-INCH			21-INCH		30-INCH	36-INCH	CLASS HE-IV 19X30-INCH		FROM	то	CLASS IV 12-INCH			CLASS IV 21-INCH	CLASS IV 24-INCH	CLASS IV 30-INCH	CLASS IV 36-INCH	CLASS HE-1
CATEGORY	STRUCTURE	STRUCTURE	LF	LF	LF	LF	LF	LF	LF	LF	CATEGORY	STRUCTURE		LF	LF	LF	LF	LF	LF	LF	LF
0010		2.2					4.0														
0010	STUB	32	 5				10				0010 CON'T	16A	16	58							
	STUB STUB	11A 15		6								16B	16A	80							
	STUB	25A						6				17 17A	18 17		 11						59
	STUB	173	5									17A 17B	17A		27						
•	1	2	210								-	17C	17B	49							
	1A	1	60									17D	17C	58							
	2	3	183									17E	17		23						
	2A	2	39				127					17F	17E	31							
	3 3A	4 3	13				127				-	17G	17E	63							
	3A 3B	э 3	13 27									17H	17G	26							
	4	5					135					17J 18	17H STUB	44 				 4			
	4A	4	13									18A	18		45						
	4B	4	27									18B	18A		110						
	5	6					262				-	18C	18	55							
	5A	5	13									18D	STUB	38							
	6	6В					26					19	20					80			
	6A	6	13									19A	19	31							
	6В 6С	/ 1A	151				33				-	19B	19	13							
	6D	6B	8									20 20A	21 20	13				85 			
	7	STUB					4					21	22					56			
	8	9					122					21A	21	31							
	8A	8	13									21B	21	13							
	8B	8	43								-	22	23					61			
	9	10					304					22A	22B	21							
	9A 10	9 11	32				256					22B	22C	28							
	10 10A	10	13				230					22C	22	 7				27			
	10B	10	31								-	22D 23	22C 24					166			
	11	12					30					23A	23	31							
	11A	11				13						24	25					183			
	11B	11A	24									24A	24	31							
	12	13					54				-	24B	24	13							
	12A	12	25 172									25	26							172	
	12В 12С	12 12в	173 43									25A	25 25		12					31	
	12D	12G	34									25B 26	25 27		13					219	
	13	STUB	4									26 26A	27 26B	19							
-	14	15	58								-	26B	26C	30							
	14A	14	58									26C	26	57							
	14B	14A	25									27	28							28	
	15	16		43								29	30	23							
	16	STUB			4							30	31	34							
										CON'T		32	34					55			
										CON I			TOTALS	2,240	278	4	10	2 000	F	450	F.O.
													IUIALS	۷,240	2/8	4	13	2,080	6	450	59

									INLET	S, MANHOLES, MA	NHOLE COVERS	S, AND INL	ET COVERS	;					
	STRUCT.				611.2005 MANHOLES 5-FT DIAMETER		INLETS 4-FT R DIAMETER		INLETS MEDIAN 1 GRATE	MANHOLE COVERS TYPE J- SPECIAL	* SPV.0060.03 INLET COVERS TYPE DW	INLET COVERS TYPE H	INLET COVERS TYPE H-S	INLET COVERS TYPE MS	INLET COVERS TYPE MS-A	CONCRETE COLLARS FOR PIPE	STORM SEWER TAP	04 650.4000 CONSTRUCTION STAKING STORM SEWER	
CATEGORY	10#	STATION	LOCATION	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	REMARKS
0010	1	76+50.00'	7.0 'RT	1					1	1				1				1	
	1A 2	77+00.01' 74+40.00'	39.9 'RT 7.0 'RT	1						1								1	
	2A	74+62.13'	25.6 'LT					1				1						1	
	3 3A	72+57.00' 72+57.00'	7.0 'RT 19.5 'RT	1				1		1		1						1	
	3B	72+57.00'	19.5 'LT					1				1						1	
	4	71+30.00'	7.0 'RT		1					1								1	
		71+30.00' 71+30.00'	19.5 'RT 19.5 'LT					1 1				1 1						1 1	
	5	69+95.00'	7.0 'RT		1					1								1	
	5A	69+95.00'	19.5 'RT					1				1						1	
	6 6A	67+33.00' 67+33.00'	7.0 'RT 19.5 'RT		1			1		1			1					1	
		67+33.00'	19.0 'LT					1					1					1	
	6C 6D	78+50.00' 67+41.00'	19.5 'RT 19.0 'LT					1 1				1	1					1	
	7	67+41.00	52.3 'LT						1						1			1	
	8	80+85.00'	7.0 'RT	1						1								1	
	8A 8B	80+85.00' 80+57.09'	19.5 'RT 26.3 'LT					1 1				1	1					1 1	
	9	82+06.00'	7.0 'RT		1					1								1	
	9A	82+05.80'	24.7 'LT				1					1						1	
	10 10A	85+10.00' 85+10.00'	7.0 'RT 19.5 'RT		1			 1		1		1						1 1	
	10B	85+10.00'	23.5 'LT					1				1						1	
	11	87+65.92'	7.0 'RT		1					1								1	
		87+65.00' 87+42.00'	19.5 'RT 19.5 'RT				1	1				1						1	
		87+68.01'	23.4 'LT		1					1								1	
		87+42.00' 89+49.72'	23.5 'LT 23.5 'LT				1	1				1						1	
		89+50.00'	19.5 'RT				1					1						1	
		89+79.40'						1					1					1	
		88+57.00' M 90+98.00'					1	1				1	1					<u>1</u> 1	
	14A	90+40.00'	20.0 'RT				1						1					1	
		90+20.62'	35.8 'RT				1					1						1	
		91+56.00' 91+56.00'	19.5 'RT 23.5 'LT					1			1		1					1	INLET COVERS TYPE DW - STAGE 1; GRATE ELEV. 1361.95
	16A	90+98.00'	23.5 'LT					1					1					1	
		90+18.00' 96+88.24' K	23.5 'LT	1				1 		 1		1						1	
		96+88.55' K					1						1					1	
	17в	96+93.00'	19.5 'RT				1					1						1	
		97+42.00' 98+00.00'	19.5 'RT 19.5 'RT					1 1				1 1						1 1	
		96+86.84' K					1						1					1	
		96+10.00'					1					1						1	
		97+50.00' K 97+67.90' K							1					1				1	
	173	98+13.25' K	33.5 'RT				1					1						1	
	18 184	96+55.00' 96+10.00'	23.5 'LT 23.5 'LT		1			 1			1	 1	1					1	INLET COVERS TYPE DW - STAGE 1; GRATE ELEV. 1360.03
		96+10.00° 95+00.00°	23.5 'LT					1				1						1	
	18C	97+10.00'	23.5 'LT					1				1						1	
	18D 19	100+40.00 F 104+30.02'			1			1		1			1					1 1	
	19A	104+30.00'	23.5 'LT					1				1						1	
		104+30.00'						1				1						1	
																		C	CON'T
PROJECT N	0• 5100)-N8-72		μν	VY:STH 33			רחויי	NTY: MC	NROF		l N	IISCELI	ANFOLI	S QUANTI	ITIFS			SHEET
I NOULUI IV	0.0100	00 12		I 174	11.0111 00			1 500	14 1 1 . IVIC	71411OL		[''	JULLL	AINLUU.	J QUANTI	IILJ			JIILLI

INLETS,	MANHOLES,	MANHOLE	COVERS,	AND	INLET	COVERS
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				611.2004	611.2005	611.2006	611.3004	611.3230	611.3901	611.0535	SPV.0060.03	611.0624	611.0639	611.0642	611.0645	520.8000	SPV.0060.0	4 650.4000	
							INLETS			MANHOLE COVERS	INLET	INLET	INLET	INLET	INLET	CONCRETE	STORM	CONSTRUCTION	
				MANHOLES	MANHOLES	MANHOLES	4-FT	INLETS	MEDIAN	TYPE J-	COVERS	COVERS	COVERS	COVERS	COVERS	COLLARS	SEWER	STAKING	
	STRUCT.			4-FT DIAMETER	5-FT DIAMETER	6-FT DIAMETER	DIAMETER	2X3-FT	1 GRATE	SPECIAL	TYPE DW	TYPE H	TYPE H-S	TYPE MS	TYPE MS-A	FOR PIPE	TAP	STORM SEWER	
CATEGORY	ID#	STATION	LOCATION	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	REMARKS
0010 CON'T	20	105+10.02'			1					1								1	
		105+10.00'						1				1						1	
		105+95.03'			1					1								1	
		105+90.00'						1				1						1	
		105+95.00'					1					1						1	
	22	106+50.00'			1					1								1	
		106+01.62'							1						1			1	
		106+31.00						1					1					1	
		106+21.64			1					1								1	
		106+15.99						1					1					1	
	23	107+10.00'		1						1								1	
		107+10.00'						1				1						1	
	24	108+75.00'		1						1								1	
		108+75.00'						1				1						1	
		108+75.00'						1				1						1	
	25	110+58.00'				1				1								1	
		110+54.00'			1							1						1	
		110+58.43'						1				1						1	
		112+30.72'			1					1								1	
		111+71.38'					1						1					1	
	26B	112+40.00					1				1							1	
	26C	112+33.00					1					1						1	
	27	114+50.00'				1				1								1	
	29	112+08.00'	19.5 'RT				1					1						1	
	30	112+27.32'					1						1					1	
	32	115+34.87'					1						1					1	
	-	67+32.34'	56.3 'LT													1			
	-	87+64.84'	25.0 'RT													1			
	-	87+73.87'	13.8 'LT													1			
		91+56.00'	25.5 'RT													1			
	-	91+56.00'	27.5 'LT													1			
	-	98+16.50'														1			
	-	96+56.80'	27.1 'LT													1			
	-	100+41.70														1	1		
		110+53.50'														1			
	-	115+35.25'	28.9 'LT													1			
			TOTALS	7	15	2	20	38	4	22	3	40	19	2	2	10	1	86	

*ADDITIONAL QUANTITIES LISTED ELSEWHERE

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE MISCELLANEOUS QUANTITIES SHEET **E**

				310.0115	612.0106	645.0111		612.0	902.s.01		
				BASE AGGREGATE OPEN-GRADED	PIPE UNDERDRAIN 6-INCH	GEOTEXTILE FABRIC TYPE DF SCHEDULE A	CATEGORY	LOCATION	SY	REMA	RKS
CATEGORY	STRUCTURE	STATION - STATI	N LOCATION	CY	LF	SY	0010	UNDISTRIBUTED	36	L BOARD 10	LOCATIONS
							0010	UNDISTRIBUTED	30 -	r ROAKD IO	LUCATIONS
0010	6в	66+33 - 67+33	LT	3	100	55					
	6D	67+33 - 68+83	LT	4	150	85					
	6A	66+33 - 68+83	RT	7	250	140		0717 5510			
	2 A	74+62 - 75+12	LT	1	50	30		SILT FENC	E SUMMARY		
	8B	80+07 - 80+63	LT	2	75	40				620 1504	(20 1520
	8A	80+35 - 80+85	RT	1	50	30				628.1504	
	9A	81+56 - 82+06	LT	1	50	30				SILT	SILT FENC
	12A	86+92 - 87+42	LT	1	50	30	CATECORY	STATION STATION	LOCATION	FENCE LF	MAINTENANG
	13	88+57 M - 88+32		1	25	15	CATEGORY	STATION - STATION	LUCATION	LF	LF
	12D	90+19 L - 90+44		1	25	15	0010	58+65.72 - 81+20.85	RT/LT	1,300	3,640
	14B	90+09 L - 90+34	L RT	1	25	15	0010	81+20.85 - 96+52.46	LT	680	907
	15	90+46 - 93+06	RT	8	260	140		96+52.46 - 106+35.08	LT	10	13
	16	90+06 - 93+06	LT	9	300	170		106+35.08 - 116+00.31	RT.	330	484
	17A	96+89 K - 97+39		1	50	30	ı	58+44.69 PH - 66+13.53 I		430	631
	17в	96+93 - 98+43	RT	4	150	85	•	WASTE SITE		800	800
	17F	94+60 - 96+10	RT	4	150	85		WASIE SITE		800	800
	18	95+05 - 98+05	LT	9	300	170			TOTALS	3,550	6,475
	19A	103+30 - 104+		3	100	55			TOTALS	3,330	0,473
	19в	103+30 - 104+		3	100	55					
	24A	107+75 - 108+		3	100	55					
	24B	107+75 - 108+		3	100	55					
	26A	111+46 - 111+		1	25	15					
	29	111+83 - 112+		1	25	15					
	32	114+35 - 115+	35 LT	3	100	55					
			TOTALS	75	2,610	1,470					

	WATER							FINISHI	NG ITEMS					
CATEGORY	STATION - STATION	624.0100 MGAL	REMARKS				625.0100	627.0200	629.0210	630.0110 SEEDING	630.0130 SEEDING	630.0140 SEEDING	630.0200	SPV.0120.01 WATER FOR
0010	59+00.00 - 81+20.85	29 82	DUST CONTROL COMPACTION	CATEGORY	STATION - STATION	LOCATION	TOPSOIL SY	MULCHING SY	FERTILIZER TYPE B CWT	MIXTURE NO. 10 LB	MIXTURE NO. 30 LB	MIXTURE NO. 40 LB	SEEDING TEMPORARY LB	SEEDED
	81+20.85 - 96+52.46	25 64	DUST CONTROL COMPACTION	0010	58+65.72 - 81+20.85 81+20.85 - 96+52.46	RT/LT RT/LT	6,850 2,600	3,450 1,700	4.4 1.7		80 20	45 25	25 8	150 58
	96+52.46 - 106+35.08	14 43	DUST CONTROL COMPACTION		96+52.46 - 106+35.08 106+35.08 - 116+00.31	RT/LT RT/LT	2,000 1,400	1,800 380	1.3		6 20	30 6	3 2	45 32
	106+35.08 - 116+00.31	13 39	DUST CONTROL COMPACTION		96+97.20 K - 98+42.54 K UNDISTRIBUTED WASTE SITE	RT/LT 	190 1,304 	739 	0.2 0.9 9.5	 200	2 13 	1 11 	4	4 29
	96+97.20 к - 98+42.54 к	2 6	DUST CONTROL COMPACTION			TOTALS	14,344	8,129	19	200	141	118	42	318
	58+44.69 PH - 66+13.53 PH	2	DUST CONTROL											

L						
	PROJECT NO:5100-08-72	HWY:STH 33	COUNTY: MONROE	MISCELLANEOUS QUANTITIES	SHEET	E

TOTAL

5

324

COMPACTION

URBAN CLASS I TYPE A CATEGORY STATION - STATION LOCATION SY 0010 58+65.72 - 81+20.85 RT/LT 4,350 81+20.85 - 96+52.46 RT/LT 1,150 96+52.46 - 106+35.08 RT/LT 340 106+35.08 - 116+00.31 RT/LT 1,100

EROSION MAT

7,070 TOTAL

RT/LT

628.2006

130

TEMPORARY DITCH CHECKS

MOBILIZATIONS EROSION CONTROL

PROJECT

5100-08-72

CATEGORY

0010

628.1905

EROSION

CONTROL

EACH

15

628.1910

EMERGENCY

EROSION CONTROL

EACH

8

MOBILIZATIONS

CATEGORY	STATION - STATION	LOCATION	628.7504 LF	
CATEGORI	STATION STATION	LOCATION		_
0010	60+27 - 61+83	RT/LT	87	
	74+60 - 74+60	RT	12	
	113+09 - 113+09	RT	30	
_				
	UNDISTRIBUTED		32	
_				
		TOTAL	161	

FIELD OFFICE TYPE C

96+97.20 K - 98+42.54 K

		642.5201
CATEGORY	PROJECT	EACH
0010	5100-08-72	1

ROCK BAGS

CATEGORY	STATION	LOCATION	628.7570 EACH
0010	64+80 R 75+17 P	RT/LT RT/LT	4
	82+85 A 88+83 M	RT/LT RT/LT	4 4
_	98+48 K	RT/LT	4
	99+34 S 96+96	RT/LT LT	4
		TOTAL	28

TEMPORARY PEDESTRIAN SURFACE ASPHALT

						644.1410.S
CATEGORY	STAGE	STATION	-	STATION	LOCATION	SF
0010	STAGE 1	59+00.00	-	81+20.85	LT	
		81+20.85	-	96+52.46	LT	740
		96+52.46	-	106+35.08	LT	275
	_	106+35.08	_	116+00.31	LT	35
		SUBTOTA	L (S	STAGE 1)		1,050
	STAGE 2	59+00.00	_	81+20.85	RT	615
		81+20.85	_	96+52.46	RT	
		96+52.46	_	106+35.08	RT	220
		106+35.08	_	116+00.31	RT	123
	•	SUBTOTA	L (S	STAGE 2)		958
	STAGE 4	59+00.00	_	81+20.85	LT/RT	
		81+20.85	_	96+52.46	LT/RT	75
		96+52.46	_		LT/RT	143
		106+35.08	_		LT/RT	75
	•		L (S	STAGE 4)	,	293
	STAGE 5	59+00.00	_	81+20.85	LT/RT	
		81+20.85	_		LT/RT	85
		96+52.46	_		LT/RT	85
		106+35.08	_		LT/RT	85
	•	SUBTOTAL			= 1, 111	255
				_	TOTAL	2,556

INLET PROTECTION

CATEGORY	STATION - STATION	LOCATION	628.7005 TYPE A EACH	628.7010 TYPE B EACH	628.7015 TYPE C EACH	628.7020 TYPE D EACH
0010	58+65.72 - 81+20.85	RT/LT	15		9	3
	81+20.85 - 96+52.46	RT/LT	24	2	21	2
	96+52.46 - 106+35.08	RT/LT	14	1	14	1
	106+35.08 - 116+00.31	RT/LT	13	2	17	
	96+97.20 K - 98+42.54 K	RT/LT	3		2	
	•	TOTALS	69	5	63	6

HWY:STH 33

PROJECT NO:5100-08-72

COUNTY: MONROE

MISCELLANEOUS QUANTITIES

SHEET

PERMANENT	SIGNING	SUMMARY

CA	ATEGORY	SIGN NUMBER STATION	LOCATION	SIGN CODE SIZE	DESCRIPTION	ORDER LINES	634.0614 POSTS WOOD 4X6-INCH 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH 16-FT EACH	634.0618 POSTS WOOD 4X6-INCH 18-FT EACH	TYPE II	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	NOTES
	0010	1-01R 58+50	STH 33, RIGHT	R2-1 24" x 30"	Speed Limit _		-	-	-	-	-	1	1	
		1-02 59+50 1-03 65+00	STH 33, RIGHT STH 33, RIGHT	W3-5 36" x 36" R2-1 24" x 30"	Speed Limit Ahead Speed Limit _	30 30	- 1	1 -	- -	5.00	9.00	-	- -	
		1-04 66+10	STH 33, RIGHT	R1-1 30" x 30"	Stop		1	-	-	5.18	-	-	-	
		1-05R 66+10 1-06 66+50	STH 33, RIGHT STH 33, LEFT	R1-1 30" x 30" J1-1 24" x 39"	Stop Junction or End Assembly (One Headed)	JCT	- 1	-	-	- 6.50	-	1	1	
3		1 00 00+30	3111 33, EE11		Suffection of the Assembly (one headed)	27	_			0.30				
		1-07R 66+50	STH 33, LEFT	J1-1 24" x 39"	Junction or End Assembly (One Headed)	JСТ 27	-	-	-	-	-	1	1	
		1-08R 66+50	STH 33, RIGHT	R2-1 24" x 30"	Speed Limit _	40	-	-	_	-	-	1	1	
		1-09 68+00	STH 33, RIGHT	R2-1 24" x 30"	Speed Limit _	30	1	-	-	5.00	-	-	-	
		1-10 69+10 1-11 69+10	STH 33, RIGHT STH 33, RIGHT	W11-2 30" x 30" W16-9P 24" x 12"	Pedestrian Crossing Ahead		-	-	_	-	6.25 2.00	-	-	
		1-12 69+80	STH 33, RIGHT	W14-3 48" x 36"	No Passing Zone Pennant		1	-	-	-	6.00	-	-	
		1-13R 69+80 1-14 70+40	STH 33, RIGHT STH 33, LEFT	W14-3 48" x 36" W14-3 48" x 36"	No Passing Zone Pennant No Passing Zone Pennant		- 1	-	- -	-	- 6.00	1 -	1	
		1-15R 70+40	STH 33, LEFT	W14-3 48" x 36"	No Passing Zone Pennant		-	-	-	-	-	1	1	
		1-16 71+10 1-17R 71+10	STH 33, LEFT STH 33, LEFT	R2-1 24" x 30" R2-1 24" x 30"	Speed Limit _ Speed Limit _	30 30	1	-	_	5.00	-	- 1	- 1	
		1-17 71-15	STH 33, RIGHT	W11-2 30" x 30"	Pedestrian Crossing	30	-	1	_	-	6.25	-	-	
		1-19 71+15	STH 33, RIGHT	W16-7L 24" x 12"	Left/Down Arrow		-	-	-	-	2.00	-	-	
		1-20 71-30 1-21 71+30	STH 33, LEFT STH 33, LEFT	W11-2 30" x 30" W16-7L 24" x 12"	Pedestrian Crossing Left/Down Arrow		-	-		-	6.25 2.00	-	-	
		1-22 73+20	STH 33, LEFT	W11-2 30" x 30"	Pedestrian Crossing		-	1	-	-	6.25	-	-	
		1-23 73+20 1-24 74+25	STH 33, LEFT STH 33, LEFT	W16-9P 24" x 12" R1-1 30" x 30"	Ahead Stop		- 1	-	- -	- 5.18	2.00	_	-	
		1-25R 74+25	STH 33, LEFT	R1-1 30" x 30"	Stop		-	-	-	-	-	1	1	
		2-01 76+10 2-02R 76+10	STH 33, RIGHT	R1-1 30" x 30" R1-1 30" x 30"	Stop		1	-	-	5.18	-	- 1	- 1	
		2-02R 76+10 2-03 76+90	STH 33, RIGHT STH 33, RIGHT	R1-1 30" x 30" R2-1 24" x 30"	Stop Speed Limit _	30	1	-	-	5.00	-	-	-	
		2-04R 76+90	STH 33, RIGHT	R2-1 24" x 30"	Speed Limit _	30	-	-	-	-	-	1	1	
		2-05 77+00 2-06R 77+00	STH 33, LEFT STH 33, LEFT	R2-1 24" x 30" R2-1 24" x 30"	Speed Limit _ Speed Limit _	30 40	1 -	-	-	5.00	- -	- 1	- 1	
		2-07 80+50	STH 33, LEFT	R1-1 30" x 30"	Stop		1	-	-	5.18	-	-	-	
		2-08R 80+50 2-09 80+70	STH 33, LEFT STH 33, RIGHT	R1-1 30" x 30" S1-1 36" x 36"	Stop School Advance		-	-	- 1	-	- 6.75	1	1	
		2-10 80+70	STH 33, RIGHT	S16-9P 30" x 18"	Ahead (Fluorescent Yellow Green)		-	-	-	-	3.75	-	- -	
		2-11 80+70	STH 33, RIGHT	R2-6P 24" x 18"	Fines Higher		-	-	-	3.00	-	-	-	
		2-12R 80+70 2-13 81+40	STH 33, RIGHT STH 33, RIGHT	S1-1 36" x 50" R1-1 30" x 30"	School Advance Assembly Stop		1	-	_	5.18	-	_	-	
		2-14R 81+40	STH 33, RIGHT	R1-1 30" x 30"	Stop		-	-	-	-	-	1	1	
		2-15 82+00 2-16R 82+00	STH 33, LEFT STH 33, LEFT	R1-1 30" x 30" R1-1 30" x 30"	Stop Stop		1 -	-	-	5.18	- -	- 1	- 1	
		2-17 83+40	STH 33, LEFT	R2-1 24" x 30"	Speed Limit _	30	-	-	1	5.00	-	-	=	
		2-18 83+40 2-19 83+40	STH 33, LEFT STH 33, RIGHT	S5-2 24" x 30" S4-51 24" x 48"	End School Zone School Speed Limit	20	-	- 1	_	5.00	- 8.00	-	-	
		2-20R 83+40	STH 33, RIGHT	54-51 24" x 48"	School Speed Limit	20	-	-	-	-	-	1	1	
		2-21R 85+00 2-22 85+40	STH 33, LEFT STH 33, RIGHT	R2-1 24" x 30" S1-1 36" x 36"	Speed Limit _ School Advance	30	-	-	-	-	- 6.75	1	1	Banded to RRFB Pole
		2-22 85+40	STH 33, RIGHT	WF16-7L 30" x 18"	Left/Down Arrow (Fluorescent Yellow Green)		-	-	_	-	3.75	-		Banded to RRFB Pole
		2-24R 85+40	STH 33, RIGHT	S1-1 36" x 54"	School Crossing Assembly		-	-	-	-	-	1	1	
		2-25 85+60 2-26 85+60	STH 33, LEFT STH 33, LEFT	S1-1 36" x 36" WF16-7L 30" x 18"	School Advance Left/Down Arrow (Fluorescent Yellow Green)		-	-	_	-	6.75 3.75	-		Banded to RRFB Pole Banded to RRFB Pole
		2-27R 85+60	STH 33, LEFT	S1-1 36" x 54"	School Crossing Assembly		-	-	-	-	-	1	1	
		2-28 86+00 2-29R 86+00	STH 33, RIGHT STH 33, RIGHT	R1-1 30" x 30" R1-1 30" x 30"	Stop Stop		1 -	-	-	5.18 -	- -	- 1	- 1	
		2-30 87+00	STH 33, LEFT	S4-51 24" x 48"	School Speed Limit	20	-	-	-	-	8.00	-	-	
		2-31R 87+00 2-32 87+00	STH 33, LEFT STH 33, RIGHT	S4-51 24" x 48" R2-1 24" x 30"	School Speed Limit Speed Limit _	20 30	- -	-	- 1	- 5.00	-	1	1	
		2-32 87+00 2-33 87+00	STH 33, RIGHT	S5-2 24" x 30"	End School Zone		-	-	-	5.00	-	-	-	
		2-34R 87+00	STH 33, RIGHT	R2-1 24" x 30"	Speed Limit _	30	-	-	-	-	-	1	1	
		2-35 87+80 2-36R 87+80	STH 33, LEFT STH 33, LEFT	R1-1 30" x 30" R1-1 30" x 30"	Stop Stop		1 -	-	-	5.18 -	-	- 1	- 1	
		2-37 88+60	STH 33, LEFT	s1-1 36" x 36"	School Advance		-	-	1	-	6.75	-	-	
		2-38 88+60 2-39 88+60	STH 33, LEFT STH 33, LEFT	S16-9P 30" x 18" R2-6P 24" x 18"	Ahead (Fluorescent Yellow Green) Fines Higher		-	-	_	3.00	3.75	-	-	
		3-01R 90+00	STH 33, RIGHT	R1-1 30" x 30"	Stop		-	-	-	-	-	1	1	
		3-02 90+20 3-03R 90+50	STH 33, RIGHT STH 33, LEFT	R1-1 30" x 30" S1-1 36" x 50"	Stop School Advance Assembly		1	-	-	5.18	-	- 1	- 1	
		3-04R 91+90	STH 33, LEFT	R2-1 24" x 30"	Speed Limit _	30	- -	-	-	-	- -	1	1	
		3-05 92+80	STH 33, RIGHT	s1-1 36" x 36"	School Advance		-	-	1	-	6.75	-	-	
		3-06 92+80 3-07 92+80	STH 33, RIGHT STH 33, RIGHT	S16-9P 30" x 18" R2-6P 24" x 18"	Ahead (Fluorescent Yellow Green) Fines Higher		-	-	-	3.00	3.75	-	-	
		3-08R 93+60	STH 33, RIGHT	J1-1 24" x 39"	Junction or End Assembly (One Headed)	JCT	-	-	-	-	-	1	1	
														CON'T
PROJI	ECT NO:	5100-08-72	HV	WY:STH 33	COUNTY: MONROE	MISCELLAN	EOUS QUAN	TITIES					SHEET	E
			DACTATI 3DASHEETSPI ANA			/2016 11 23 ΔM PLOT F	RY . JACK ROBERT	Δ PI 0.1			PLOT SCALE .			

FILE NAME: S:\MAD\1000--1099\1089\837\ACAD\CIVIL 3D\SHEETSPLAN\030201_MQ.DWG LAYOUT NAME - 030201_MQ - 030211A_MQ

PERMANENT	CTCNTNC	CHMMADV	CON'T

	CATEGORY	SIGN NUMBE	R STATION	LOCATION	SIGN CO	DE SIZE	DESCRIPTION	ORDER LINES	POSTS WOOD 4X6-INCH 14-FT EACH	POSTS WOOD 4X6-INCH 16-FT EACH	POSTS WOOD 4X6-INCH 18-FT EACH	SIGNS TYPE II REFLECTIVE H SF	SIGNS TYPE II REFLECTIVE F SF	REMOVING SIGNS TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH	NOTES	
1	0010 CON'T	3-09	94+00	STH 33, RIGHT	J1-1	24" x 39"	Junction or End Assembly (One Headed)	JCT II	1	-	-	6.50	-	-	-		
		3-11R	94+50	STH 33, RIGHT	s1-1		school Advance Assembly	Ü	-	-	-	-	-	1	1		t
		3-12R 3-13	94+50 94+50	STH 33, RIGHT STH 33, RIGHT	R1-1 R1-1	30" x 30"	Stop Stop		-	-	-	5.18	-	1	1		
		3-14 3-15	95+00 95+00	STH 33, LEFT STH 33, LEFT	R2-1 S5-2		Speed Limit _ End School Zone	30	-	-	1	5.00 5.00	-	-	-		
		3-16	95+00	STH 33, RIGHT	S4-51	24" x 48"	School Speed Limit	20	-	1	-	-	8.00	-	-		ı
		3-17R 3-18	96+10 96+10	STH 33, RIGHT STH 33, RIGHT	S1-1 S1-1		School Crossing Assembly School Advance		-	_	-	-	- 6.75	1 -	1	Banded to RRFB Pole	
		3-19	96+10	STH 33, RIGHT	s16-7L	. 30" x 18"	Left/Down Arrow (Fluorescent Yellow Green)			-		-	3.75	-	-		
4		3-20 3-21	96+20 96+20	STH 33, LEFT STH 33, LEFT	S1-1 S16-71	36" x 36" . 30" x 18"	School Advance Left/Down Arrow (Fluorescent Yellow Green)		-		-	-	6.75 3.75	_	_	Banded to RRFB Pole	ŀ
		3-22R	96+20	STH 33, LEFT	S1-1	36" x 54"	School Crossing Assembly		-	-	-		-	1	1		- 1
		3-23 3-24R	96+70 97+10	STH 33, RIGHT STH 33, LEFT	R1-1 R2-1	30" x 30" 24" x 30"	Stop Speed Limit _	30	1 -	_	- -	5.18	- -	- 1	- 1		
		3-25	97+50	STH 33, LEFT		24" x 48"	School Speed Limit	20	-	1	-	-	8.00	-	-		
		3-26R	97+60	STH 33, RIGHT	J3-1		Directional Assembly With Cardinals (One Headed)	East, 33, Right and Ahead Combination Arrow	w -	-	=	=	=	1	1		
		3-27R 3-28	97+90 98+00	STH 33, LEFT STH 33, RIGHT	J4-1 J3-2		Reassurance Assembly (One Headed) Directional Assembly With Cardinals (Two Heads)	West, 33 (Blank), East	-	_	- 1	- 19.00	-	1 -	1 -		-
		5 20	-0.00	55, KIGH	33 2	" "		U, 33			-	23.00					
		3-29R	98+40	STH 33, RIGHT	R1-1	30" x 30"	Stop	(Forward Arrow), (Right Arrow)	_	_	_	_	_	1	1		
		3-30	98+80	STH 33, LEFT	J4-1		Reassurance Assembly (One Headed)	West	1	-	-	6.00	-	-	-		
		3-31	99+10	STH 33, RIGHT	s1-1	36" x 36"	School Advance	33	-	1	_	_	6.75	_	_		
		3-32	99+10	STH 33, RIGHT	s16-7L	. 30" x 18"	Left/Down Arrow (Fluorescent Yellow Green)		-	-	-	-	3.75	-	-		
		3-33R 3-34R	99+20 99+20	STH 33, RIGHT STH 33, RIGHT	S1-1 J3-2		School Crossing Assembly Directional Assembly With Cardinals (Two Heads)		-	_	-	-	- -	1	1		
		3-35	99+60	STH 33, LEFT	S1-1		School Advance		-	1	-	-	6.75	-	-		
		3-36 3-37R	99+60 99+60	STH 33, LEFT STH 33, LEFT	S16−7L S1−1		Left/Down Arrow (Fluorescent Yellow Green) School Crossing Assembly		-	_	-	-	3.75 -	1	1		
		3-38	99+65	STH 33, LEFT	R1-1	30" x 30"	Stop		1	-	-	5.18	-	-	-		
		3-39r 3-40r	99+65 99+65	STH 33, LEFT STH 33, LEFT	R1-1 J3-2		Stop Directional Assembly With Cardinals (Two Heads)		-	_	-	-	-	1	1		
		3-41R	100+40 100+40	STH 33, LEFT	R1-1		Stop		-	-	-	- F 10	-	1	1		
		3-42 3-43R	100+40	STH 33, LEFT STH 33, RIGHT	R1-1 J4-1		Stop Reassurance Assembly (One Headed)	East, 33	_	-	-	5.18	_	1	1		
		3-44	100+50	STH 33, RIGHT	J4-1	24" x 36"	Reassurance Assembly (One Headed)	East 33	1	-	=	6.00	=	-	-		-
		4-01	101+90	STH 33, LEFT	J3-2	24" x 57"	Directional Assembly With Cardinals (Two Headed)	West, (Blank)	-	-	1	19.00	-	-	-		
								33, U Left Arrow, Right Arrow									
		4-02R	102+50	STH 33, LEFT		24" x 57"	Directional Assembly With Cardinals (One Headed)	Lere Arrow, Right Arrow	-	-	-	-	-	1	1		
		4-03 4-04	103+00 104+00	STH 33, LEFT STH 33, RIGHT	W3-1 R2-1	36" x 36" 24" x 30"	Stop Ahead Speed Limit _	30	=	1 -	- 1	- 5.00	9.00	_	-		
		4-05	104+00	STH 33, RIGHT	s5-2	24" x 30"	End School Zone		-	-	-	5.00	-	-	-		
		4-06R 4-07	104+00 104+05	STH 33, RIGHT STH 33, LEFT	R2-1 S4-51	24" x 48"	Speed Limit _ School Speed Limit	30 20	-	- 1	-	-	- 8.00	1 -	1 -		
		4-08R	104+05	STH 33, LEFT	w3-1	36" x 36"	Stop Ahead		-	=	-	-	-	1	1		
		4-09 4-10	105+50 105+50	STH 33, LEFT STH 33, LEFT	S1-1 S16-9P		School Advance Ahead (Fluorescent Yellow Green)		-	-	1	-	6.75 3.75	-	-		
		4-11	105+50	STH 33, LEFT	R2-6P	24" x 18"	Fines Higher		-	-	-	3.00	-	-	-		
		4-12R 4-13R	105+50 106+50	STH 33, LEFT STH 33, RIGHT	S1-1 R1-1		School Advance Assembly Stop		-	_	-	-	-	1	1		
		4-14 4-15	106+50 106+60	STH 33, RIGHT	R1-1	30" x 30" 24" x 39"	Stop Junction or End Assembly (One Headed)	1CT	1	_	_	5.18 6.50	- -	_	_		
				STH 33, LEFT				JCT U	1	-	-	0.30	_	-	=		
		4-16R 4-17R	106+60 108+00	STH 33, LEFT STH 33, RIGHT	J1-1 R2-1		Junction or End Assembly (One Headed) Speed Limit _	JCT, U 30	-	- -	-	-	_	1	1 1		
		4-18	108+00	STH 33, LEFT	R2-1	24" x 30"	Speed Limit _	30	1	-	-	5.00	-	-	-		
1		4-19R 4-20	108+00 111+00	STH 33, LEFT STH 33, RIGHT	R2-1 R2-1		Speed Limit _ Speed Limit _	30 30	- 1	- -	-	- 5.00	- -	1 -	1 -		1
		4-21R	111+80	STH 33, LEFT	R1-1	30" x 30"	Stop	50	-	-	-	-	-	1	1		
		4-22 4-23R	111+80 112+60	STH 33, LEFT STH 33, RIGHT	R1-1 R1-1		Stop Stop		1 -	-	-	5.18	- -	- 1	- 1		
		4-24	112+60	STH 33, RIGHT	R1-1	30" x 30"	Stop		1	-	-	5.18	-	-	-		
		5-01 5-02R	112+80 112+80	STH 33, RIGHT STH 33, RIGHT	W14-3 W14-3		No Passing Zone Pennant No Passing Zone Pennant		-	1 -	-	-	6.00 -	- 1	- 1		
		5-03	113+50	STH 33, LEFT	R2-1	24" x 30"	Speed Limit _	30 30	1	-	-	5.00	-	-	-		
		5-04r 5-05r	113+50 115+50	STH 33, LEFT STH 33, LEFT	R2-1 R1-1		Speed Limit _ Stop	30	-	- -	-	-	_	1 1	1 1		
		5-06	115+50	STH 33, LEFT	R1-1	30" x 30"	Stop		1	-	-	5.18	-	-	-		
		5-07 5-08r	98K+10 98K+50	KISSEL AVENUE, RIG KISSEL AVENUE, LE			Stop Stop		1 -	_	-	5.18	-	- 1	- 1		
		5-09	98K+50	KISSEL AVENUE, LE		30" x 30"	Stop		1	-	-	5.18	-	-	-		
								TOTALS	34	13	10	259.92	214.00	56	56	_	
				<u>.</u>											•	<u>.</u>	_
PRC	DJECT NO:	5100-08-	-72		HWY: STH	1 33	COUNTY: MONROE	MISCELLANEOUS	QUANTIT	ΓIES					SHEET	l E	∃

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			DURATION _	TRAFFIC DR	.0300 CONTROL UMS	TRAFFIC CC	045.01 CONTROL	TRAFFI BARF TYF	3.0420 C CONTROL RICADES PE III	TRAFFIC WARNING TYF	.0715 CONTROL G LIGHTS PE C	TRAFFIC SI	.0900 CONTROL	TRAFFIC SIC PC	GNS :MS
CATEGORY	STAGE	STATION - STATION	(DAYS)	EACH	DAYS	EACH	DAYS	EACH	DAYS	EACH	DAYS	EACH	DAYS	EACH	DAYS
0010	STAGE 1	59+00.00-81+20.85 81+20.85-96+52.46	10 10	97 61	970 610	0	0 0	24 17	240 170	10 0	100 0	16 13	160 130	1	10
		96+52.46-106+35.08	10	37	370	0	0	3	30	0	0	10	100		
		106+35.08-119+00.00	10	47	470	0	0	14	140	6	60	13	130	1	10
	•	SUBTOTALS (STAGE 1)			2,420		0		580		160		520		20
	STAGE 2/3	59+00.00-81+20.85 81+20.85-96+52.46	44 44	10 0	440 0	77 61	3,388 2,684	22 20	968 880	9 0	396 0	16 13	704 572		
		96+52.46-106+35.08	44	0	0	39	1,716	15	660	0	0	15	660		
		106+35.08-119+00.00	44	7	308	36	1,584	18	792	0	0	15	660		
	-	SUBTOTALS (STAGE 2)			748		9,372		3,300		396		2,596		0
	STAGE 4	59+00.00-81+20.85 81+20.85-96+52.46	12 12	24 0	288 0	154 121	1,848 1,452	27 26	324 312	0	0 0	32 29	384 348		
		96+52.46-106+35.08	12	0	0	74	888	9	108	0	0	14	168		
		106+35.08-119+00.00	12	9	108	71	852	20	240	5	60	28	336	1	12
	-	SUBTOTALS (STAGE 4)			396	, -	5,040		984		60		1,236		12
	STAGE 5	59+00.00-81+20.85	40	6	240	77	3,080	21	840	0	0	15	600		
		81+20.85-96+52.46	40	0 0	0	61	2,440	20	800 720	0	0 0	13 11	520 440		
		96+52.46-106+35.08	40 40	7	280	40 39	1,600 1,560	18 20	720 800	0 5	200	15	600		
	-	106+35.08-119+00.00 SUBTOTALS (STAGE 5)	40		520	39	8,680	20	3,160	3	200		2,160		0
	STAGE 6	96+52.46-106+35.08 106+35.08-119+00.00	10 10	23 24	230 240	0	0	3 1	30 10	0 0	0 0	6 3	60 30		
	-	subtotals (stage 6)			470		0		40		0		90		0
		UNDISTRIBUTED											330		
	•	TOTALS			4,554		23,092		8,064		816		6,932		32

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE MISCELLANEOUS QUANTITIES SHEET **E**

CATECORY	CTATION	T0	CTATION	LOCATION	EPOXY 4-INCH	CROSSWALK EPOXY 6-INCH	647.0556 PAVEMENT MARKING STOP LINE EPOXY 12-INCH	DEMARKS
CATEGORY	STATION	TO	STATION	LOCATION	LF	LF	LF	REMARKS
0010	59+20	_	78+00	STH 33	2548	_	_	YELLOW CENTERLINE
	78+00	_	106+00	STH 33	5600	_	_	DOUBLE YELLOW CENTERLINE
	106+00	_	112+00	STH 33	750	_	_	YELLOW CENTERLINE
	112+00	_	116+00	STH 33	100	_	-	DASHED YELLOW CENTERLINE
	59+20	_	99+55	STH 33, LT	4212	-	-	WHITE EDGELINE
_	100+39	-	116+00	STH 33, LT	1561	-	-	WHITE EDGELINE
	61+87	-	99+50	STH 33, RT	3763	-	-	WHITE EDGELINE
	100+43	-	116+00	STH 33, RT	1557	-	-	WHITE EDGELINE
		71+18		STH 33	-	36	_	WHITE CROSSWALK
_		71+24		STH 33	-	36	_	WHITE CROSSWALK
		85+48		STH 33	-	40	_	WHITE CROSSWALK
		85+54		STH 33	-	40	-	WHITE CROSSWALK
		96+19		STH 33	-	40	-	WHITE CROSSWALK
		96+25		STH 33	-	40	-	WHITE CROSSWALK
_		99+52		STH 33	-	50		WHITE CROSSWALK
		99+58		STH 33	-	56	-	WHITE CROSSWALK
		99's'+58		SOUTH STREET	-	44	-	WHITE CROSSWALK
		99's'+64		SOUTH STREET	-	52	-	WHITE CROSSWALK
		00'F'+2	-	FRONT STREET	-	48	-	WHITE CROSSWALK
_	1	00'F'+3	2	FRONT STREET	-	43		WHITE CROSSWALK
		100+34		STH 33	-	61	-	WHITE CROSSWALK
		100+40		STH 33	-	54	-	WHITE CROSSWALK
	Ć	99's'+54	ļ	SOUTH STREET	-	-	22	WHITE STOP LINE
		100+45		STH 33	-	-	12	WHITE STOP LINE
				TOTALS	20,091	640	34	

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE MISCELLANEOUS QUANTITIES SHEET **E**

	TEMPODARY	DEDECTRIAN ACCOMMODATION	CLIMMADY				-	-MDOD A DV	DAVEMEN	IT MADICANC	_				ADJUST W	WATER VALVE	
	TEMPORARY I	PEDESTRIAN ACCOMMODATION	SUMMARY	_			ı	EMPORARY	PAVEMEN	T MARKING	.						SPV.0060.05
		644.1420.S	644.1430.s							6	549.0400	64	19.0403	CATEGORY	STATION	LOCATION	EACH
		TEMPORARY	TEMPORARY							REMOVAB	LE TAPE 4-INC	CH EPOX	(Y 4-INCH				
		PEDESTRIAN	PEDESTRIAN							(WHITE)	(DOUBLE YEL	LOW) (DOUB	LE YELLOW)	0050	65+50	15' RT	1
		SURFACE PLYWOOD	SURFACE PLATE		CATEGORY	STAGE	STATION - STAT	ION LOCA	ATION	LF	LF		LF		65+56	21' RT	1
CATEGORY	LOCATIO	N SF	SF												70+62 72+22	31' RT 24' LT	1
0010			250		0010	STAGE 1	59+00.00 - 81+20		/RT		490		3,860		72+22 73+19	24 LT 24' LT	1 1
0010	UNDISTRIBU	TED 250	250				81+20.85 - 96+52		/RT				3,090		74+24	15' RT	1
							96+52.46 - 106+3		/RT				1,850		74+30	10' RT	1
							106+35.08 - 116+0		/RT		410		1,930		74+83	37' LT	1
							SUBTOTAL (STAGE	1)		0	900]	10,730		75+13	15' RT	1
	TEMPORA	Y CURB RAMP	_			CTACE 3	FO:00 00 01:30		· /p.=	240	500		2 070		77+00	23' RT	1
						STAGE 2	59+00.00 - 81+20		/RT	240	500		3,870		77+29	37' LT	1
		644.1601.s					81+20.85 - 96+52		/RT				3,030		78+43	37' LT	1
CATEGORY	LOCATIO	N EACH	_				96+52.46 - 106+3		/RT		410		1,850		79+04	37' LT	1
							106+35.08 - 116+0		/RT	240	410		1,930		80+71	15' RT	1
0010	UNDISTRIBU	TED 4					SUBTOTAL (STAGE	2)		240	910	_	10,680		80+75	9' RT	1
						CTACE F	59+00.00 - 81+20	\ OF 1=	· /p.=		4 100				80+75 81+34	23' RT 19' RT	1
						STAGE 3	81+20.85 - 96+52		/RT /RT		4,160 3,080				81+34 81+78	19 KT 35' LT	1
							96+52.46 - 106+3		/RT		1,770				82+33	15' RT	1
	TEMI	PORARY PEDESTRIAN SAFETY	FENCE				106+35.08 - 116+0		/RT		2,230				82+39	10' RT	1
							SUBTOTAL (STAGE		/ K I	0			0		82+44	42' LT	1
			644.1	516.S			JUBIUTAL (STAGE	4)		U	11,240		U		84+07	35' LT	1
CATEGORY	STAGE	LOCATION	L	=					ΓALS	240	13,050		21,410		85+00	34' LT	1
								101	IALS	240	13,030	2	11,410		85+38	35' LT	1
0010	STAGE 2/3	PARK STREET	9)				TTEM	TOTAL		13,290				85+67	23' RT	1
		SOUTH STREET/FRONT S	TREET 7	5				TICM	TOTAL		13,230				86+82	35' LT	1
		ONTARIO AVENUE SO													87+69	15' RT	1
		SUBTOTAL (STAGE 2	/3) 20	9			CONST	RUCTION S	TAKTNO						87+72	22' RT	1
				_			CONST	NOCITON 3	IAKING						93+28	37' LT	1
	STAGE 4	MAIN STREET	2					650.450	0 650	.5000	650.7000 6	50.9920			94+44	35' LT	1
		SOUTH STREET/FRONT S										SLOPE			95+20 96+04	36' LT 12' RT	1
		ONTARIO AVENUE SOI						SUBGRAD	E B			STAKES			96+04	35' LT	1
		SUBTOTAL (STAGE	4) 9	_	CATEGORY	STA	TION - STATION	LF	1	LF	LF	LF			96+48	9' RT	1
	_		_	_								_			96+52	14' RT	1
	STAGE 5	MAIN STREET	3		0010		0.00 - 81+20.85	1,930	-		1,930	2,220			99+81	9' RT	1
		SOUTH STREET/FRONT S					0.85 - 96+52.46	1,530	-			1,530			99+85	5' RT	1
		ONTARIO AVENUE SOI					2.46 - 106+35.08	985		·	985	985			99+85	13' RT	1
		SUBTOTAL (STAGE	5) 9	3			5.08 - 116+00.31	965		65 34	900	965			103+20	32' LT	1
			TOTAL A				00 R - 65+48.88 R 38 W - 75+35.00 W	64		24 45	40 16	64 61			105+02	32' LT	1
			TOTAL 40	3			34 P - 75+83.62 P	61 62		43 37	25	62			104+88	28' RT	1
							35 G - 81+50 G	43		17	26	43			106+29	41' RT	1
							00 H - 81+03.05 H	28		13	15	28			106+30	20' RT	1
	D EMOV	ING PAVEMENT MARKINGS					58 A - 82+80.00 A	26		11	15	26			106+34	15' RT	1
	KEMOV	ING PAVEMENT MARKINGS					52 M - 88+80.00 M	77		58	19	77			106+30	32' LT	1
			646.0600			89+32.2	L7 L - 90+32.82 L	100		41	59	100			106+97	20' RT	1
CATEGORY	STAGE	STATION - STATION	LF			96+70.4	18 к - 98+42.54 к	170	1	.43	27	170			108+05	32' LT	1
						99+38.8	36 S - 99+70.62 S	32		5	27	32			111+87	43' LT 28' LT	1
0010	STAGE 1	58+65.72 - 81+20.85	7,564				00 F - 100+49.27 F			6	21	27			111+89 112+42	28 LT 30' LT	1
		81+20.85 - 96+52.46	3,058				35 J - 106+41.95 J	36		7	29	36			112+42	16' LT	<u>1</u> 1
		96+52.46 - 106+35.08	1,770				36 0 - 112+45.00 0			12	18	30			114+50	30' LT	1
		106+35.08 - 116+00.31	1,713				3 OS - 112+14.66 O			12	21	33			115+54	32' LT	1
						TT2+80.8	34 H - 116+18.95 H	32		32		32			115+57	28' LT	1
		TOTAL	14,105				TOTALS	6,231	Г	28	5,703	6,521					
							TOTALS	0,231	3		5,705	0,041			•	TOTAL	54
PROJECT N	IO• 5100 - 09	-72 I I I I	WY:STH 33		00	UNTY: MC	NIDOE		MISOF	LLANEOU	S QUANTITIE	C				SHEET	1.
PROJECT N		- (Z			100	UNITEMU		C-201C 0-17		LLANEUU	3 QUANTITIE	.S		DLOT SCALE		J SUEE !	E

Γ								Electrical Co	ndui t								
- 1										652. 0225							
										Conduit Rigid Nonmetallic							
										Schedul e 40							
										2-Inch							
							Category	Station to	Stati on	LF	Description						
							0030	72 + 33 to	72 + 24	10	LC1 to PB3						
							0030	72 + 24 to	71 + 45	81	PB3 to LB6						
							0030	71 + 45 to	69 + 55	191	LB6 to LB5	Ιiαh	ting Bases				
							0030	69 + 55 to	67 + 65	191	LB5 to LB4	Līgn	ing bases		654 0101	654. 0105	I
-1		Pull Boxe	s		,	•	0030 0030	67 + 65 to 66 + 02 to	66 + 02 $65 + 41$	164 62	LB4 to PB2 PB2 to PB1						
					SPV. 0060. 01		0030	65 + 41 to	65 + 37	6	PB1 to LB3					Concrete	
					Pull Boxes		0030	65 + 37 to	63 + 46	192	LB3 to LB2				Bases	Bases	
3					Non-Conductive	:	0030	63 + 46 to	61 + 91	156	LB2 to LB1	Catagomy Stati	D:	Lacation	Type 1	Type 5	Dogoni nti on
'					24"x42"		0030 0030	72 + 33 to 72 + 42 to	72 + 42 73 + 38	11 97	LC1 to PB4 PB4 to LB7	Category Stati	on Dir	Locati on	EACH	EACH	Descri pti on
	Category	Station	Di r	Locati on	EACH	Description	0030	72 + 42 = 10 $73 + 38 = 10$	75 + 38 75 + 28	190	LB7 to LB8	0000	0.4	00.4			T D4
							0030	75 + 28 to	75 + 59	32	LB8 to PB5	0030 61 +		26. 1		1	LB1
	0030	65 + 41	RI GHT	31. 6	1	PB1	0030	75 + 59 to	76 + 41	83	PB5 to PB6	0030 63 +		27. 3		1	LB2
	0030	66 + 2	RI GHT	25. 6	1	PB2	0030	76 + 41 to	77 + 18	78	PB6 to LB9	0030 65 +		27. 5		1	LB3
	0030	72 + 24	RI GHT	38. 2	1	PB3	0030 0030	77 + 18 to 79 + 08 to	79 + 08 80 + 94	191 187	LB9 to LB10 LB10 to LB11	0030 67 +		24. 7		1	LB4
	0030	72 + 42	RI GHT	37. 8	1	PB4	0030	80 + 94 to	80 + 94 80 + 98	5	LB10 to LB11 LB11 to PB7	0030 69 +		23. 5		1	LB5
	0030	75 + 59	RI GHT	22. 9	1	PB5	0030	80 + 98 to	81 + 52	55	PB7 to PB8	0030 71 +		23. 8		1	LB6
	0030	76 + 41	RI GHT	22. 7	1	PB6	0030	81 + 52 to	83 + 17	166	PB8 to LB12	0030 73 +		24. 0		1	LB7
	0030	80 + 98	RI GHT	31. 4	1	PB7	0030	101 + 16 to	101 + 03 99 + 41	27	LC2 to PB18	0030 75 +		24. 0		1	LB8
	0030	81 + 52	RI GHT	22. 2	1	PB8	0030 0030	101 + 03 to 99 + 41 to	99 + 41 99 + 20	325 22	PB18 to PB16 PB16 to LB20	0030 77 +		30. 9		1	LB9
	0030	89 + 73	RI GHT	30. 7	1	PB9	0030	99 + 20 to	96 + 86	235	LB20 to PB15	0030 79 +		29. 4		1	LB10
	0030	90 + 47	RI GHT	21. 9	1	PB10	0030	96 + 86 to	96 + 31	56	PB15 to PB14	0030 80 +	94 RI GHT	31. 0		1	LB11
	0030	91 + 29	LEFT	27. 6	1	PB11	0030	96 + 31 to	95 + 27	105	PB14 to LB18	0030 83 +	17 RI GHT	22. 3		1	LB12
	0030	93 + 2	RI GHT	22. 6	1	PB12	0030 0030	95 + 27 to 93 + 02 to	93 + 02 91 + 04	226 199	LB18 to PB12 PB12 to LB16	0030 85 +	47 RI GHT	22. 7		1	LB13
	0030	95 + 43	LEFT	27. 8	1	PB13	0030	91 + 04 to	90 + 47	58	LB16 to PB10	0030 87 +	15 RI GHT	21. 7		1	LB14
	0030	96 + 31	RI GHT	31. 0	1	PB14	0030	90 + 47 to	89 + 73	75	PB10 to PB9	0030 88 +	95 LEFT	27. 6		1	LB15
	0030	96 + 86	RI GHT	32. 1	1	PB15	0030	89 + 73 to	87 + 15	259	PB9 to LB14	0030 91 +	4 RI GHT	29. 0		1	LB16
	0030	99 + 41	RI GHT	28. 9	1	PB16	0030	87 + 15 to	85 + 59	157	LB14 to PB30	0030 93 +	21 LEFT	27. 1		1	LB17
	0030	99 + 38	LEFT	25. 8	1	PB17	0030 0030	99 + 41 to 99 + 38 to	99 + 38 97 + 42	55 197	PB16 to PB17 PB17 to LB19	0030 95 +	27 RI GHT	29. 5		1	LB18
			RI GHT	30. 2	1	PB18	0030	97 + 42 to	95 + 43	200	LB19 to PB13	0030 97 +	42 LEFT	27. 7		1	LB19
	0030		RI GHT		1	PB19	0030	95 + 43 to	93 + 21	223	PB13 to LB17	0030 99 +	20 RI GHT	30. 5		1	LB20
	0030	101 + 25 101 + 28	LEFT	30. 1 26. 5	1	PB20	0030	93 + 21 to	91 + 29	193	LB17 to PB11	0030 100 +	30 LEFT	33. 1		1	LB21
	0030				1		0030	91 + 29 to	88 + 95	234	PB11 to LB15	0030 102 +		25. 6		1	LB22
	0030	102 + 74	RI GHT	22. 5	1	PB21	0030 0030	101 + 16 to 101 + 25 to	101 + 25 101 + 28	19 57	LC2 to PB19 PB19 to PB20	0030 104 +		29. 7		1	LB23
	0030	104 + 61	LEFT	26. 5	1	PB22	0030		100 + 30	99	PB20 to LB21	0030 106 +		25. 7		1	LB24
	0030	106 + 16	RI GHT	30. 7	1	PB23	0030	101 + 28 to	102 + 43	116	PB20 to LB22	0030 109 +		29. 8		1	LB25
	0030	106 + 62	RI GHT	30. 0	1	PB24	0030	102 + 43 to	104 + 61	219	LB22 to PB22	0030 111 +		25. 9		1	LB26
	0030	109 + 20	LEFT	26. 6	1	PB25	0030	104 + 61 to 106 + 82 to	106 + 82 109 + 20	222 239	PB22 to LB24 LB24 to PB25	0030 113 +		30. 6		1	LB27
I	0030	111 + 59	LEFT	25. 6		PB26	0030 0030		111 + 39	239 220	PB25 to LB26	0030 115 +		30. 6		1	LB28
	0030	112 + 18	LEFT	22. 3	1	PB27	0030	111 + 39 to		21	LB26 to PB26	0040 85 +		27. 7	1	1	SB1
	0040	85 + 59	RI GHT	22. 5	1	PB30	0030		112 + 18	60	PB26 to PB27	0040 96 +		28. 2	1		SB2
	0040	85 + 61	LEFT	26. 6	1	PB31	0030		113 + 38	121	PB27 to LB27	0040 96 +		22. 1	1		SB3
	0040	96 + 32	LEFT	26. 6	1	PB32	0030 0030	113 + 38 to 101 + 25 to	115 + 38 102 + 74	200 150	LB27 to LB28 PB19 to PB21	0040 001	12 KI GIII	<i>‰</i> . 1	1		БВО
							0030	101 + 23 to 102 + 74 to	102 + 74 $104 + 37$	164	PB21 to LB23	-					
							0030	104 + 37 to	106 + 16	180	LB23 to PB23			Total	3	20	
				Total	30		0030		106 + 62	47	PB23 to PB24			Total	ا ع	28	I
							0030 0030	106 + 62 to 85 + 59 to	109 + 16 85 + 47	255 13	PB24 to LB25 PB30 to LB13						
							0030	85 + 59 to 85 + 59 to	85 + 47 85 + 61	13 50	PB30 to LB13 PB30 to PB31						
							0040	96 + 31 to	96 + 32	58	PB14 to PB32						
							0040	96 + 32 to	96 + 22	11	PB32 to SB2						
							0040	96 + 31 to	96 + 12	21	PB14 to SB3						
							0040	85 + 61 to	85 + 56	6	PB31 to SB1						
ŀ									Total	7542		•					
	STATE	PROJECT	NO: 510	00-08-72		HWY: S	TH 33		COUNT	Y: MONROI	Ξ	MISCELLANEOUS	QUANTITIE	S	SHEE	T NO:	E
_	FILE NAME .	:				DI OT DATE :		DI OT	BY:		PLOT NAME :	000000		ORIGINATO	R · DIST		PLOT SCALE : 1:1
	I ILL INAIVIE					FLOI DATE:		FLUI	٠١		I LOT NAIVIL	ORG DATE :		ONIGINATO	· . DIST _		I LOT SUALE . I.I

				657. 0100	657. 0255	657. 0420	657. 0322	657. 0710	659. 1120	ſ
				Pedestal	Transformer			Lumi nai re	Lumi nai res	
				Bases	Bases	Si gnal	Pol es	Arms Truss	Utility	
				Dases	Breakaway	Standards	Type 5-	Type	LED	
				1	11 1/2-Inch		Al umi num	4. 5-Inch Clamp	В	
				1	Bolt Circle		THE CHILL FROM	12- FT	D	
Category	Station	Di r	Locati on	EACH	EACH	EACH	EACH	EACH	EACH	Desc
earegory	Scacion	DII	Location	Laten	Liten	Laten	Liten	Laton	LHOH	Desc
0030	61 + 91	RI GHT	26. 1		1		1	1	1	LB1
0030	63 + 46	RI GHT	27. 3		1		1	1	1	LB2
0030	65 + 37		27. 5		1		1	1	1	LB3
0030	67 + 65	RI GHT	24. 7		1		1	1	1	LB4
0030		RI GHT	23. 5		1		1	1	1	LB5
0030	71 + 45		23. 8		1		1	1	1	LB6
0030		RI GHT	24. 0		1		1	1	1	LB7
0030	75 + 28		24. 0		1		1	1	1	LB8
0030		RI GHT	30. 9		1		1	1	1	LB9
0030	79 + 8	RI GHT	29. 4		1		1	1	1	LB10
0030	80 + 94		31. 0		1		1	1	1	LB11
0030	83 + 17	RI GHT	22. 3		1		1	1	1	LB12
0030	85 + 47	RI GHT	22. 7		1		1	1	1	LB13
0030	87 + 15	RI GHT	21. 7		1		1	1	1	LB14
0030	88 + 95	LEFT	27. 6		1		1	1	1	LB15
0030	91 + 4	RI GHT	29. 0		1		1	1	1	LB16
0030	93 + 21	LEFT	27. 1		1		1	1	1	LB17
0030	95 + 27	RI GHT	29. 5		1		1	1	1	LB18
0030	97 + 42	LEFT	27. 7		1		1	1	1	LB19
0030	99 + 20	RI GHT	30. 5		1		1	1	1	LB20
0030	100 + 30	LEFT	33. 1		1		1	1	1	LB21
0030	102 + 43	LEFT	25. 6		1		1	1	1	LB22
0030	104 + 37	RI GHT	29. 7		1		1	1	1	LB23
0030	106 + 82	LEFT	25. 7		1		1	1	1	LB24
0030	109 + 16	RI GHT	29. 8		1		1	1	1	LB25
0030	111 + 39	LEFT	25. 9		1		1	1	1	LB26
0030	113 + 38	LEFT	30. 6		1		1	1	1	LB27
0030	115 + 38	LEFT	30. 6		1		1	1	1	LB28
0040	85 + 56	LEFT	27. 7	1		1				SB1
0040	96 + 22	LEFT	28. 2	1		1				SB2
0040	96 + 12	RI GHT	22. 1	1		1				SB3
			Total	3	28	3	28	28	28	l

Poles, Arms & Equipment

	Electi	ri cal	items							
					656. 0200. 01	656. 0200. 02	650. 8500	654. 0224	659. 2124	
					El ectri cal	El ectri cal	Construction	Concrete	Li ghti ng	
					Servi ce	Servi ce	Staki ng	Control	Control	
					Meter Breaker	Meter Breaker	El ectri cal	Cabi net	Cabi nets	
					Pedestal	Pedestal	Installations	Bases	120/240	
					East Cabi net	West Cabinet	5100-08-72	Type L24	24- i nch	
Category	Station	n	Di r	Locati on	LS	LS	LS	EACH	EACH	Description
0030	72 +	33	RI GHT	42. 2	1			1	1	LC1
0030	101 +	16	RI GHT	30. 5		1		1	1	LC2
0030	72 +	33	RI GHT	42. 2			1			Proj ect
				Total	1	1	1	2	2	

STATE PROJECT NO: 5100-08-72 HWY: STH 33 COUNTY: MONROE MISCELLANEOUS QUANTITIES SHEET NO: E

FILE NAME : ______ PLOT DATE : _____ PLOT BY : _____ PLOT NAME : _____ ORG DATE : _____ ORIGINATOR : DIST PLOT SCALE : 1:1

Street	Light	Wire
DULEEL	LISIL	11110

	Street Li	gnt v	wire	1			1
				655. 0610	655. 0615	655. 0620	
				El ectri cal		El ectri cal	
				Wi re	Wi re	Wi re	
				Li ghti ng	Li ghti ng	Li ghti ng	
				12 AWG	10 AWG	8 AWG	
Category	Stati on	to	Station	LF	LF	LF	Descri pti on
0030	072 + 33	to	061 + 91			3213	LC1 to LB1
0030	072 + 33	to	083 + 17			3375	LC1 to LB12
0030	101 + 16	to	085 + 47			4869	LC2 to LB13
0030	101 + 16	to	088 + 95			3936	LC2 to LB15
0030	101 + 16	to	100 + 30			546	LC2 to LB21
0030	101 + 16	to	115 + 38			4575	LC2 to LB28
0030	101 + 16	to	109 + 16			2490	LC2 to LB25
0040	101 + 16	to	096 + 12		1617		LC2 to SB3
0040	096 + 12	to	096 + 22		345		SB3 to SB2
0040	101 + 16	to	085 + 47		4890		LC2 to LB13
0040	085 + 47	to	085 + 56		243		LB13 to SB1
0030	061 + 91			150			LB1 Up Pole
0030	063 + 46			150			LB2 Up Pole
0030	065 + 37			150			LB3 Up Pole
0030	067 + 65			150			LB4 Up Pole
0030	069 + 55			150			LB5 Up Pole
0030	071 + 45			150			LB6 Up Pole
0030	073 + 38			150			LB7 Up Pole
0030	075 + 28			150			LB8 Up Pole
0030	077 + 18			150			LB9 Up Pole
0030	079 + 08			150			LB10 Up Pole
0030	080 + 94			150			LB11 Up Pole
0030	083 + 17			150			LB12 Up Pole
0030	085 + 47			150			LB13 Up Pole
0030	087 + 15			150			LB14 Up Pole
0030	088 + 95			150			LB15 Up Pole
0030	091 + 04			150			LB16 Up Pole
0030	093 + 21			150			LB17 Up Pole
0030	095 + 27			150			LB18 Up Pole
0030	097 + 42			150			LB19 Up Pole
0030	099 + 20			150			LB20 Up Pole
0030	100 + 30			150			LB21 Up Pole
0030	102 + 43			150	1		LB22 Up Pole
0030	104 + 37			150			LB23 Up Pole
0030	106 + 82			150			LB24 Up Pole
0030	109 + 16			150			LB25 Up Pole
0030	111 + 39			150			LB26 Up Pole
0030	113 + 38			150			LB27 Up Pole
0030	115 + 38			150			LB28 Up Pole
							<u> </u>
			Total	4200	7095	23004	

RRFB SPV. 0060. 02
Rapi d
Rect angul as

Rectangul ar Fl ashi ng

Beacon

		Deacon	
Category	Station	EACH	Description
0040	85 + 52	2. 0	Elementary School
0040	96 + 25	2. 0	Kissel Avenue
	Total	4. 0	

STATE PROJECT NO: 5100-08-72 HWY: STH 33 COUNTY: MONROE MISCELLANEOUS QUANTITIES SHEET NO: E

FILE NAME : ______ PLOT DATE : _____ PLOT BY : _____ PLOT NAME : _____ ORG DATE : _____ ORIGINATOR : DIST PLOT SCALE : 1:1

		SAW	ING						INLET CO	OVERS TYP	PE DW		CONS	TRUCTION S	STAKING CUR	3 RAMP
				* 690.0150 ASPHALT	* 690.0250 CONCRETE		CATEGORY	LOCATION	* SPV.006 EACH		REN	MARKS	CATEGORY	STATION	LOCATION	SPV.0060.06 EACH
CATEGORY 0010		- STATION - 81+20.85	LOCATION LT/RT	LF 245	155		0010	110+55 LT	1			STING STRUCTURE	0010	65+50 65+90	53' RT 48' RT	1 1
	81+20.85 96+52.46	- 96+52.46 - 106+35.08		480 535	120 330			115+36 LT	2		STAGE 1 EXIS	STING STRUCTURE		71+22 71+22	20' RT 20' LT	1 1
	96+97.20 K	- 116+00.31 - 98+42.54 OTALS	LT/RT K LT/RT	175 120 1,555	150 55 810		*ADDITIONAL	QUANTITIES		EWHERE				74+32 74+57	34' LT 34' LT 24' LT	1 1
	UNDIST	RIBUTED		80	40									74+72 74+72 75+75	24 LT 20' RT 29' RT	1 1
	SUBT	OTALS		80	40				TEMPORA	ARY SURFA	ACE SAME DAY			76+23 76+32	30' RT 22' RT	1
			TOTALS	1,635	850		CATEGORY	STAGE	LOCAT	ION	SPV.0180.01 SY	L REMARKS		76+32 80+60	24' LT 40' LT	1
*ADDITIONA	L QUANTITIES	S LISTED ELS					0010	STAGE 1	87+68	LT/RT		STORM SEWER CROSSING		80+89 81+00	40' RT 25' RT	1
			SAWING		* 690.0150	* 690.0250	_		89+50 91+57 96+12	LT LT/RT RT	17 47 7	STORM SEWER CROSSING STORM SEWER CROSSING STORM SEWER CROSSING		81+40 82+11	25' RT 31' LT	1 1
CATEGORY	STAGE	STATION -	- STATION	LOCATION	ASPHALT LF	CONCRETE LF	_	-	96+58 96+92	LT/RT RT	47 7	STORM SEWER CROSSING STORM SEWER CROSSING		82+52 85+51 85+51	31' LT 20' RT 24' LT	1 1
0010	STAGE 1	59+00.00 - 81+20.85 -		LT/RT LT/RT	895 1,830	68 170		_	SUBTOTAL (STAGE 1)	240			87+52 87+54	20' RT 24' LT	1 1
			- 106+35.08	LT/RT LT/RT	1,080 870	71 85		STAGE 2	67+33 71+30	LT LT	30 20	STORM SEWER CROSSING STORM SEWER CROSSING		87+67 88+03	36' LT 31' LT	1 1
		SUBTOTALS		. = /==	4,675	394	_		72+57 74+50 80+71	LT LT LT	20 27 32	STORM SEWER CROSSING STORM SEWER CROSSING STORM SEWER CROSSING		89+71 90+31	25' RT 25' RT	1 1
	STAGE 2/3	59+00.00 - 81+20.85 - 96+52.46 -		LT/RT LT/RT LT/RT	2,210 1,630 1,080	36 72 		-	82+06 85+11	LT LT	21 20	STORM SEWER CROSSING STORM SEWER CROSSING		96+22 96+22 96+32	22' RT 24' LT 28' RT	1 1
	-	106+35.08 -		LT/RT	1,250 6,170	108	-		104+30 105+92	LT LT	20 21	STORM SEWER CROSSING STORM SEWER CROSSING		96+77 99+47	28' RT 24' RT	1
	STAGE 4	59+00.00 - 81+20.85 -		LT/RT LT/RT	1,940 1,520	24 48		-	107+10 108+76 110+54	LT LT	21 19	STORM SEWER CROSSING STORM SEWER CROSSING		99+47 99+59	30' RT 40' RT	1 1
	_	96+52.46 - 106+35.08 -	- 106+35.08 - 116+00.31	LT/RT	1,010 980		_		112+22 115+36	LT LT LT	45 21 45	STORM SEWER CROSSING STORM SEWER CROSSING STORM SEWER CROSSING		99+67 100+19 F 100+29 F		1 1 1
	STAGE 5	SUBTOTALS 59+00.00		LT	5,450 150	72 20		_	SUBTOTAL (STAGE 2)	361			100+27 100+40	30' LT 24' LT	1 1
	51//GE 5		- 96+52.46	LT	150	40	-			TOTAL	601			106+03 106+03	20' RT 24' LT	1 1
				TOTALS	16,445	634	=							106+16 106+57 111+75	25' RT 25' RT 35' LT	1 1
*ADDITIONA	L QUANTITIES	S LISTED ELS	EWHERE											111+73 112+05 112+11	34' LT 22' LT	1 1 1
CONCRET	E PAVEMENT 3	JOINT LAYOUT												112+15 112+23	20' RT 26' RT	1 1
		SPV.0105.01												115+46 97+69 K		1 1
CATEGORY		LS												98+16 K 98+25 K 98+25 K	16' RT	1 1 1
0010 5	100-08-72	1												55.25 K	TOTAL	52
PROJECT NO	D : 5100-08-	72	Н	WY:STH 33			COUNTY: MONRO)E		MISCELL	ANEOUS QUA	NTITIES			SHEET	

4

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET PROJECT NO. 5100-08-22

CASHTON - HILLSBORO STH 27 TO V CASHTON EVL

STH 33 MONROE COUNTY

R 3 W

CONVENTIONAL SYMBOLS

CORNER

SECTION LINE OLIARTER LINE SIXTEENTH LINE NOTATION FOR COMBUSTABLE CAUTION NOTATION FOR NEW REFERENCE LINE FLUIDS NEW R/W LINE EXISTING R/W LINE NOTATION FOR HIGH VOLTAGE PROPERTY LINE LOT, TIE & OTHER MINOR LINES CORPORATE LIMITS 111111111 ELECTRIC POLE UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC) TELEPHONE POLE FEE ACQUISITION AREA (HATCHING VARIES BY OWNER) PEDESTAL (LABEL TYPE) (TV. TEL. ELEC. ETC.) TEMPORARY LIMITED EASEMENT AREA ACCESS CONTROLLED BY ACQUISITION NO ACCESS (BY STATUTORY AUTHORITY)

4

EASEMENT AREA (HIGHWAY, RESTRICTED DEVELOPMENT) TRANSMISSION STRUCTURES BUILDING

NATIONAL GEODETIC SURVEY MONUMENT 🛆 SIXTEENTH CORNER MONUMENT

PROJECT OR CONTROL) PARCEL NUMBER

ACCESS RESTRICTED (BY PREVIOUS

(40) UTILITY NUMBER

R/W MONUMENT

FOUND IRON PIN

WATER, ETC.)

VALVE (GAS.

COMPENSABLE

SIGN

NON-MONUMENTED O R/W POINT

OFF-PREMISE FICE

sign

NON-COMPENSABLE

占

Д

CONVENTIONAL ABBREVIATIONS

POINT OF CURVATURE ACCESS RIGHTS POINT OF COMPOUND CURVE PCC AHEAD POINT OF INTERSECTION ALUMINUM PROPERTY LINE AND OTHERS ET AL RECORDED AS (100') BACK REFERENCE LINE R/L BL OCK BLK REMAINING REM CENTERI THE RIGHT RT CERTIFIED SURVEY MAP CSM RIGHT OF WAY R/W CONCRETE CONC SECTION SEC SEPTIC VENT COUNTY CO SEPV COUNTY TRUNK HIGHWAY CTH SHED DISTANCE DIST SQUARE FEET CORNER STATE TRUNK HIGHWAY STH COR DOCUMENT NUMBER DOC STATION STA EASEMENT EASE SUBDIVISION SUBD TANGENT TAN EXISTING TELEPHONE PEDESTAL GAS VALVE TEMPORARY LIMITED GRID NORTH TLE HIGHWAY EASEMENT EASEMENT TRANSPORTATION PROJECT HOUSE IDENTIFICATION UNITED STATES HIGHWAY LAND CONTRACT VOLUME MONUMENT CURVE DATA NATIONAL GEODETIC SURVEY NGS LONG CHORD NUMBER LONG CHORD BEARING OUTL OT RADIUS PAGE DEGREE OF CURVE POINT OF TANGENCY CENTRAL ANGLE OR DELTA PERMANENT LIMITED PLF LENGTH OF CURVE EASEMENT

T 15 N

CONVENTIONAL UTILITY

SYMBOLS

GAS TELEPHONE

FLECTRIC

TRANSMISSION I INFS

CABLE TELEVISION

FIBER OPTIC SANITARY SEWER

STORM SEWER

SEC.19 SEC.23 SEC.24 SEC.20 SEC.25 T 15 N SEC.30 SEC.29 . SEC.26 Cashton SEC.32 SEC.36 SEC.35 SEC.31 R 3 WR 4 W

R 4 W

PLOT DATE: 12/14/2012 7:46 PM

LAYOUT 0.5 MI.

PROJECT LOCATION

THE NOTES, CONVENTIONAL SYMBOLS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 5100-08-22.

DISTANCES SHOWN FROM THE RIGHT-OF-WAY LINE TO BUILDINGS ARE APPROXIMATE ONLY.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD83 (1991) IN U. S. SURVEY FEET, VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBAR) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD". PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES, THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

ALL RIGHT-OF-WAY LINES DEPICTED IN NON-ACQUISITION AREAS ARE INTENDED TO REESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENT.

DIMENSIONING FOR THE NEW R/W IS MEASURED ALONG AND PERPENDICULAR TO NEW REFERENCE LINES. FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION LACROSSE OFFICE.

PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS & INTERESTS REQUIRED.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES. AS DEFINED HEREIN. INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. ALL TEMPORARY LIMITED EASEMENTS (TLES) EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHT TO MAKE OR CONSTRUCT IMPROVEMENT ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 33 IS ESTABLISHED FROM PREVIOUS PROJECTS 5100-07-21, 5143-2-21 AND 5143-1-21, EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 27 IS ESTABLISHED FROM PREVIOUS PROJECT 5143-2-21.

EXISTING ACCESS CONTROL ALONG STH 33 IS ESTABLISHED FROM PREVIOUS PROJECT 5143-1-21 AND

EXISTING ACCESS CONTROL ALONG STH 27 IS ESTABLISHED FROM PREVIOUS PROJECT 5143-2-21.

RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 5100-08-22-4.01 SHEET 2 OF 2

FILE NAME : G:\WDOTSW\14023-004\CIVIL3D\SHEETSPLAN\RW\51000822_401_TITLE.DWG

POB

TANGENT

DIRECTION AHEAD

DIRECTION BACK

PLOT BY: SCOTT DEMEUSE

POINT OF BEGINNING

ACCEPTED FOR RECORDING AND

STATION OFFSET TABLE

STATION

POINT

100

101

102

103

104

105

106

107

108

109

110

111

112

113

213.39' L

103.53' L

51.42' L

50.69' I

OFFSET

PLE/TLE STATION OFFSET TABLE									
POINT	STATION	OFFSET							
T150	66+40.65	69,99' R							
T151	66+09.94	69.43' R							
P152	58+84.27	169.02' R							
P153	58+88.62	195,34' R							
P154	58+65.78	209,86' R							
P155	58+64.52	203.85' R							

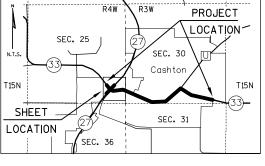
60+56.07

61+13.39

64+07.91

65+91.04

LOCATION SKETCH



Engineering

200

. THOMAS J. HANSEN. PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.01 AND THAT SUCH PLAT CORRECTL REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

Than I Han 09-29-2015 THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033

THIS PLAT AND RELOCATION ORDER ARE APPROVED. FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

Cory Schlage CORY SEHLAGEL

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.01

PART OF LOT 7, BLOCK 1, NORTH PARK ADDITION; AND PART OF LOT 10, PLAT OF BLOCK 4 OF NORTH PARK ADDITION; ALL LOCATED IN THE SE 1/4 - SE 1/4 OF SECTION 25, T15N, R4W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HERBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

1984.68 TO R/W

MATCH

13(201)

TLE SLOPES

LOT 7

BLOCK 1

NORTH PARK

ADDITION

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NADR3 (1991) IN ILS SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE

LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

SCHEDULE OF LANDS & OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER INTERESTS REQUIRED

OF LAND INTEREST TO D.O.T.

ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

PARCEL		INTEREST	R/W A	ACRES REQ	UIRED	TLE	PLE
NUMBER	OWNER(S)	REQUIRED	NEW	EXISTING	TOTAL	ACRES	ACRES
1	JEROME L. & LORI A. LEIS	TLE				0.01	
69	GVC, INC.	PLE					0.02

PLF COURSE TABLE COURSE BEARING DISTANCE P152-P153 S00° 00' 00"E 27.45 P153-P154 N90° 00' 00"W 45 16' P154-P155 N00° 00' 00"E 6.47' P155-P152

CURVE 102-P152	CURVE P155-P152	CURVE P103-155
L=693.45'	L=49.80'	L=679,24'
LC=682.92'	LC=49.79'	LC=669.35'
LCB=S83°39'30"W	LCB=N65°04'54"W	LCB=N46°51'34"E
R=1146.19'	R=1146.19'	R=1146.19'

	COURSE TABLE					
COURSE	BEARING	DISTANCE				
100-101	SEE CURVE	DATA				
101-102	N78° 19' 38 " W	61.43'				
102-103	SEE CURVE	DATA				
103-104	N60° 07' 02 " W	201.95				
104-105	SEE CURVE	DATA				
105-106	N36° 22' 21"E	205,60'				
106-107	N20° 43' 26"E	101.19'				
107-108	N27° 20' 53"E	247.14				
108-109	S77° 28' 16"E	134.81'				
109-110	S73° 19' 12"E	223,45				
110-111	S36° 41' 13"E	124.11'				
111-112	S80° 40' 06"E	321.30'				
112-113	SEE CURVE DATA					
113-100	S00° 40' 39"E	119,04'				

CURVE 100-101	CURVE 102-103	CURVE 104-105	CURVE 112-113
L=26.92'	L=1422.49'	L=511,78'	L=192,92'
LC=26.92'	LC=1332.94'	LC=460,45'	LC=192.63'
LCB=N75°16'03"W	LCB=S65°26'12"W	LCB=N37°05'46"E	LCB=S75°20'49"E
R=1147.09'	R=1146.19'	R=3520.11'	R=1004.93'

∠S47°31'24"E FRONT STREET √N81°02'26"È STH 33 PI: 58+92.78 102 IP 3/4" 101 PLE SIDEWALK 100(200) LOT 1 T151 CSM T150 V. 12, P. 213 P152 LOT 11 DOC. #479533 *₽153* ROAD <u>|P154</u> LOT 10

LOT 14

CASHTON

NORTH PARK ADDITION

LOT 12 | LOT 13

KENNEDY LANE

LOT 2

LOT 8

HIGHWAY

STH 33

STH 27

FOUND HARRISON MON.

Y = 307,867.417

X = 631.554.434

BASIS

(25|30)

715 N

LOT 1

CSM

V. 21, P. 61

DOC. #574504

66+10.17. 0.00' RT Y=305,828.518 X=631.578.546

PREVIOUS PROJECT'S 5143-02-21 & 5143-01-21

PREVIOUS PROJECT 5143-02-21

FILE NAME : G:\WDOTSW\14023-004\CIVIL3D\SHEETSPLAN\RW\040101_RP.DWG

N89°55'17"E 1216.51'

SEC. COR. TO R/L

103

FOUND HARRISON MON.

= 305,227,167

= 628,930,481

STH 27

DELTA=13°42'34" RT

BK TAN=N29°29'33"E

55+44.36, 0.00' RT

Y=306.089.434

X=630,609.606

PI=50+36.41

D=01°30'28"

T=456.81

L=909.25'

R=3800-001

PC=45+79.60

PT=54+88.85

SW-SE

48+63.60, 0.00' RT

15 N

²⁵

36

(STH 27)

Y=305,284.809

X=630,183.714

Y=305,440.674

X=630,259.684

STH 33

Y=305.844.650

X=630,857.244

DELTA=51°26'11" LT

PI=58+92.78

D=24°41'47"

T=111.751

L=208.27

R=232.00'

VILLAGE

S/*

\$7.5 Sq.488.85 • 106

SE-SE

LOT 8

LOT 7

S89°55'17"W 133.81'

R/W TO R/L

LOT 9

RECORDED AS SHEET 2 OF 2

PLAT OF BLOCK 4 OF

NORTH PARK ADDITION

STA. 47+96.65 (27)

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET,

PC=57+81.03

PT=59+89.30

PI=63+84.06

D=05°59'58"

T=274.571

L=534.71'

R=955.00'

STA. 49+54.15

59+57.55, 0.00' RT

(STH 27)

Y=306,115,317

X=630,893.262

109

LOT 1

CSM

V. 21, P. 59

DOC. #574503

PC=61+09.49

PT=66+44.20

Y=305,923.530

X=631,357.560

DELTA=32°04'49" RT

AH TAN=S66°52'45"E

v

STY

OF

KATIE LANE

S89°55'17"W 1304.82'

SEC. COR. TO R/W

S89°55'17"W 2655.14

SEC. COR. TO SEC. COR.

PI=55+43.79

PI=57+33.97

Y=305.951.891

X=630,740.114

Y=306,089.847

X=630,609.214

DELTA=04°41'19" LT

DELTA=04°01'36" LT

PLOT DATE: 29/09/2015

FOUND HARRISON MON.

305,230,811

X = 631.585.615

RANDY

60'

LOT 6

LOT 3

LOT 7

V. 12. P. 213

DOC. #479533

PLOT BY : KL ENGINEERING

PLOT SCALE : ########

5100-08-22-4.01

NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING LINIT OF THE

EXISTING ACCESS CONTROL ALONG STH 33 IS ESTABLISHED FROM PREVIOUS PROJECT 5143-01-21 & DOC. 529957. EXISTING ACCESS CONTROL ALONG STH 27 IS ESTABLIISHED FROM PREVIOUS PROJECT 5143-02-21.

SEE CURVE DATA

SCALE, FEET

COURSE TABLE FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE PI=63+84-06 PI=80+99.24 OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY AS SHEET 2 OF 2 DISTANCE Y=305.923.530 Y=305,244.368 COURSE BEARING OF DOCUMENT NUMBER 652481 AND FILED IN MF295B. X=631.357.560 X=632.948.240 200-201 N00° 40' 39"W 119.04 DELTA=32°04'49" RT DELTA=23°46'24" LT 201-202 SEE CURVE DATA D=05°59'58" D=10°03'07" 202-203 S66° 55' 05"E 58.36 T=274.57' T=119.98' FOUND HARRISON MON. 203-204 S48° 55' 51"F 16.01 L=534.71' L=236.51 = 307,867.417 204-205 S67° 49' 41"E 66.40 R=570-00 R=955-00 X = 631.554.434205-206 364.46' S67° 49' 41"E PC=61+09.49 PC=79+79.26 PT=66+44.20 PT=82+15.77 206-207 S19° 15' 38"W 10 47 BK TAN=S81°02'26"W AH TAN=N89°20'50"E 281,03' 207-208 S66° 25' 28"E 208-209 S66° 32' 56"E 40.59 209-210 S66° 27' 39"F 435 65' 210-211 S71° 28' 28"E 147.59' SCALE, FEET 211-212 S00° 31' 09"E 86.71 100 200 212-213 SEE CURVE DATA STATION OFFSET TABLE N66° 59' 41"W 213-214 217,73' STATION OFFSET POINT 214-215 N01° 08' 30"E 7.69' 200 66+35.75 59.75' R **VILLAGE** 215-216 N67° 12' 42"W 831.96' 201 65+91.04 50,69' L T253 216-200 SEE CURVE DATA 202 66+39.93 50.49' L 203 66+98.06 50.52' L CURVE 201-202 CURVE 212-213 CURVE 216-200 204 67+13.29 45.59' L L=265,80' L=51.48' L=152.19' 46 69' I 205 67+79 68 LC=51.48' LC=244,22' LC=152.08' SEC. 206 71+44.09 52.72' L LCB=N71°38'32"W LCB=S68°22'47"E LCB=N70°47'41"W R=1004.93' R=1507.11' R=1147.10 LOT 1 207 71+44.79 42.27' L CSM 208 74+25-82 40.04' L V. 21, P. 61 209 74+66.40 39.81' L DOC. #574544 210 79+02.04 36.63' L 211 43.79' L S66°52'45"E 34.29' 80+55-20 212 80+77.55 40.03' R ALONG TAN. 213 78+40.55 51.26' R FRONT STREET 214 76+22-82 51.70' R T25 215 76+19.95 44,57' R <u>T256</u> 216 67+88.00 49.40' R TLE STATION OFFSET TABLE OFFSET OF POINT STATION T250 69.99' R 66+40.65 T251 67+88.04 70,00' R T252 74+15-00 40-19' I SL OPES T253 56.07' L 74+15.00 SW-SW T254 74+62.40 63.95' L ASSESSOR'S PLAT T255 74+73.41 44₋69' L VILLAGE OF CASHTON 76+43.01 43.45' L T256 T257 76+43.05 48.52' L 0.L.45 T258 76+63.77 48.37' L T259 76+65.98 43.32' L LOT 7 BLOCK 1 REBAR T260 79+01.86 41.63' L SI OPES NORTH PARK ADDITION T261 48-88' L STH 33 80+53-60 O.L. 42 T262 76+44.84 51.67' R FRONT STREET T263 76+44.89 79.67' R O.L.41 LOT 3 O.L. 40 LOT 5 LOT 2 T264 83.60' R 76+35.70 55 S66°52'45"E 91.11' LOT 1 ALONG TAN. S00°31'09"E 52.89' R/W TO TAN. KATIE LANE <u>T260</u> ر 15 م CSM 30 UTILITY & INGRESS <u> T263</u> OUND PK. NAIL V. 11. P. 71 & EGRESS EASEMEN 31 = 305,277.216 DOC. #45425 \<u>T264</u>`Z X = 634,257.787 LOCATION SKETCH LOT 1 STA. 81+07.87 SLOPES 3 (315)(212) S00°31'09"E 33.82 LOT 1 TAN. TO R/W NW-NW STREE 6, P. 113

DOC. #380440

CASHTON

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.02

PART OF LOT 7, BLOCK 1, NORTH PARK ADDITION; PART OF LOT 1, CSM, V. 6, P. 113, DOC. NO. 380440; PART OF OUTLOTS 40, 41, 42, 43 & 44, ASSESSOR'S PLAT VILLAGE OF CASHTON, ALL IN THE SW 1/4 -SW 1/4; AND ALSO PART OF THE SW 1/4 - SW 1/4, ALL IN SECTION 30, T15N, R3W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN.

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE RECUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

ACCEPTED FOR RECORDING AND FILING IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROF COUNTY, WISCONSIN, AT

2:11 P.M. ON NOV. 5, 2015 AS DOCUMENT # 653050 AND FILED IN MAP FILE 296A

RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 5100-08-22-4.02

EASEMENT 1	T,	ΑI	Βl	_E
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OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL NO.	
VERNON COMMUNICATIONS COOPERATIVE	NO EASEMENT OF RECORD FOUND	3	

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED	
104	VERNON COMMUNICATIONS COOPERATIVE	RELEASE OF RIGHTS	

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES SCHEDULE OF LANDS & ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO D.O.T INTERESTS REQUIRED ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

PARCEL NUMBER	* * * * * * * * * * * * * * * * * * * *		R/W . NEW	ACRES REQ	UIRED TOTAL	TLE ACRES
1	JEROME L. & LORI A. LEIS	TLE				0.06
3	SCENIC BLUFFS COMMUNITY HEALTH CENTER, INC.	TLE				0.01
20	JEFFREY J. & NORM J. FLINT	TLE				0.01
53	CALEB E. HUNDT	TLE				0.01
54	DAVID A. & RHONDA K. ROALKVAM	TLE				0.01
55	DAVID A. & REBECCA C. PIEPER	TLE				0.01
56	T5 VENTURES LLC	TLE				0.02
57	JAMES & JANET CHAPIEWSKY	TLE				0.02
58	CASHTON FARM SUPPLY, LTD.	TLE				0.02

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD83 (1991) IN U.S SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION. CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

FOUND PROPERTY PIPES ARE 1" IRON PIPES UNLESS OTHERWISE NOTED. EXISTING ACCESS CONTROL ALONG STH 33 IS ESTABLISHED FROM PREVIOUS PROJECT 5143-02-21.

Engineering

. THOMAS J. HANSEN. PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.02 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



Than I Han THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033 THIS PLAT AND RELOCATION ORDER ARE APPROVED. FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

Cory Schlage 10/27*/*2015 CORY SCHLAGEL

PROJECT LOCATION: SEC. 25 T15N T15N

SEC. 31

PLOT DATE: 13/10/2015

V. 6. P. 113

DOC. #380440

N89°00'18"E 1442.86

R/W TO SEC. COR.

\80+67.72, 0.00' RT

Y=305,263.183

X=632,921.620

PLOT SCALE : *********

FOUND HARRISON MON.

= 305,230.811

= 631,585,615

N89°00'18"E 1229.72

SEC. COR. TO R/W

HIGHWAY

STH 33

VERNON COMMUNICATIONS

COOPERATIVE

PREVIOUS PROJECT 5143-01-21

SEC. 36

SHEET

LOCATION

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

EASEMENT TABLE

VILLAGE

MRS. C. ENGEBRETSON'S

ADDITION

N84°20'50"E

COUNTY, LLC

PARCELS 5&6

C. ENGEBRETSON'S

SUBDIVISION

RECORDING INFORMATION

EASEMENT DOC. NO. 637710

NO EASEMENT OF RECORD FOUND

NO EASEMENT OF RECORD FOUND

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

FOUND PROPERTY PIPES ARE 1" IRON PIPES UNLESS OTHERWISE NOTED.

OWNER

ST

FOUND HARRISON MON.

= 305,230.811

= 631,585,615

(211)316

τ 15 *λ*

715 1

COUNTY, LLC PARCELS 5&6

CENTURYTEL OF MONROE

CENTURYTEL OF MONROE COUNTY, LLC

VERNON COMMUNICATIONS, LLC

WE ENERGIES - GAS

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.03, AMENDMENT NO. 1

THIS REMOVES PARCEL 35 AND ADDS PARCEL 71 TO TRANSPORTATION PROJECT PLAT 5100-08-22-4.03, RECORDED AS DOC. NO. 653051 AND FILED IN MAP FILE 296B IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY.

PART OF LOT 1, CSM, V. 6, P. 73, DOC. NO. 378198; AND PART OF OUTLOT 30, ASSESSOR'S PLAT, VILLAGE OF CASHTON; ALL LOCATED IN THE NE 1/4 - NW 1/4 OF SECTION 31; PART OF LOT 1, C. ENGEBRETSON'S SUBDIVISION; PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION; PART OF OUTLOTS 36, 37, 38, 39 & 46, ASSESSOR'S PLAT, VILLAGE OF CASHTON; PART OF LOTS 2 & 3, CSM, V. 6, P 72, DOC. NO. 378197; AND PART OF LOTS 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18 & 19, BLOCK 1, SMITH'S ADDITION; ALL LOCATED IN THE SE 1/4 - SW 1/4, ALL IN SECTION 30, T15N, R3W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN,

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

Accepted for recording and filing in The office of the register of deeds in Monroe County, Wisconsin AT 3:00 P.M. ON FEB. 8, 2016 AS

DOCUMENT # 654821 AND FILED IN MAP FILE 298B

RESERVED FOR REGISTER OF DEEDS

PROJECT NUMBER 5100-08-22-4.03 AMENDMENT NO. 1

UTILITY INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES

UTILITY INTEREST LITHLITY OWNER(S) 103 RELEASE OF RIGHTS 104 VERNON COMMUNICATIONS COOPERATIVE RELEASE OF RIGHTS 105 CENTURYTEL OF MONROE COUNTY, LLC RELEASE OF RIGHTS

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY AS SHEET 2 OF 2 OF DOCUMENT NUMBER 652481 AND FILED IN MF295B. PI=80+99.24

Y=305,244.368 X=632.948.240 DELTA=23°46'24" LT D=10°03'07" T=119.98' NO0°21'35"W 62.89' L=236.51 R/L TO R/W R=570.00 95+40.74. 0.00' RT PC=79+79.26 PT=82+15.77 Y=305,658.291 . 335(423) BK TAN=N66°52'45"W X=634,255,394

300(422)

IP.

- LEWIS

WE ENERGIES - GAS

STREET

CASHTON

0.L.31

6

ASSESSOR'S PLAT

VILLAGE OF CASHTON

PARCEL 9

0.L.30

- N34°15'13"F

V. 6, P. 72

DOC. #378197

N00°21'35"W 321.32'

SEC. COR. TO R/W

FOUND P.K. NAIL

= 305,277.216

CSM

V. 13. P. 195

DOC. #491555

30

FOUND HARRISON MON.

Y = 310.599.661

X = 634,224.675

BASIS

STH 33 PREVIOUS PROJECT 5143-01-21

(45**`**

(46

OF

LOT 1

CSM

V. 6, P. 73

DOC. #378198

S89°00'18"W 2672.58'

SEC. COR. TO SEC. COR.

LOCATED IN R/W PARCEL NO.

5-6

5.6

SEE SHEET 2 OF 2 FOR COURSE TABLES, OFFSET

TABLES, CURVE INFORMATION AND DETAILS.

SE-SW

ASSESSOR'S PLAT

VILLAGE OF CASHTON

NE-NW

VERNON COMMUNICATIONS

COOPERATIVE

PARCELS 5&6

VERNON COMMUNICATIONS COOPERATIVE

CENTURYTEL OF MONROF

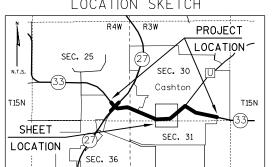
PI=88+82.05 Y=305,253,326 N00°21'35"W 59.76' X=633,734.452 DELTA=32°13'42" LT D=11°07'31" T=148.78' L=289.68' R=515_00' PC=87+33-26 PT=90+22.94

PI=93+95.69 Y=305,536.463 X=634,172.433 DELTA=22°51'55" LT D=13°10'17" T=87.971 L=173.60' R=435.00' PC=93+07.72 PT=94+81.32

SCHEDULE OF LANDS & ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER INTERESTS REQUIRED OF LAND INTEREST TO D.O.T. ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

PARCEL			INTEREST	R/W ACRES REQUIRED			TLE
	NUMBER	OWNER(S)	REQUIRED	NEW	EXISTING	TOTAL	ACRES
	5	VILLAGE OF CASHTON	FEE,TLE	0.01		0.01	0.02
	6	CASHTON AREA SCHOOL DISTRICT	FEE,TLE	0.03		0.03	0.35
	7	REBECCA A. CLEMENTS	TLE				0.01
	9	LINUS J. SCHREIER	FEE,TLE	0.02		0.02	0.06
	71	HUNDT IMPLEMENT, LLC.	FEE	0.01		0.01	
	38	RICHARD E. EIRSCHELE	TLE				0.01
	39	CARMEN K. MLSNA	TLE				0.01
	40	LUCY & ERNEST A. SCHMITZ, LIFE ESTATE, ELIZABETH JEAN SCHMITZ, KENNETH LEE SCHUTTEMEIR, JUDITH ANN WILLIAMS & KEITH ELMER SCHUTTEMEIR	FEE,TLE	0.01		0.01	0.02
	41	RITA A. BYERS	FEE,TLE	0.01		0.01	0.01
	43	SHAUN M. MICKELSON	FEE,TLE	0.02		0.02	0.04
	44	CYNTHIA L. SOLTAU & MARK W. CONNELLY	TLE				0.04
	45	SPENCER W. & SANDRA RIPLEY	FEE,TLE	0.01		0.01	0.03
	46	LUCILLE H. DICKMAN, LIFE ESTATE, MARY JO E. MCDANIEL, DIANA J. THURSTON, STEPHEN C., JEROME R. & JOHN DICKMAN, KRISTI L. FISCHER	FEE,TLE	0.01		0.01	0.01
	47	MARTIN E. & ANGEL M. MYTAS	FEE,TLE	0.01		0.01	0.03
	48	VICTOR A. GUTIERREZ & LINDA S. HORSWILL	TLE				0.01
	49	CHARLES H. & BETTY J. CUMMINGS	TLE				0.02
	50	BRYAN J. DEMPSEY & KIMBERLY A. MILLER	TLE				0.02
	51	BRIAN C. & JODY C. BRUEGGEN	TLE				0.02
	52	GARY K. LARSON	FEE,TLE	0.01		0.01	0.02
		•	•				•

LOCATION SKETCH





THOMAS J. HANSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84,095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.03 AMENDMENT NO. 1 AND THAT SUCHPLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



Thom I Han THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033 THIS PLAT AND RELOCATION ORDER ARE APPROVED. FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATIO

CORY SCHLAGEL

PLOT DATE: 01/02/2016

SCALE, FEET

100

31

PLOT NAME :

PLOT SCALE : **********

PARCELS 5&6

ACCEPTED FOR RECORDING AND

FILING IN THE OFFICE OF THE

REGISTER OF DEEDS IN MONROE

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.03

PART OF LOT 1, CSM, V. 6, P. 73, DOC. NO. 378198; AND PART OF OUTLOT 30, ASSESSOR'S PLAT, VILLAGE OF CASHTON; ALL LOCATED IN THE NE 1/4 - NW 1/4 OF SECTION 31; PART OF LOT 1, C. ENGEBRETSON'S SUBDIVISION; PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION; OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION; PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION; OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION; PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION PART OF LOT 1, MRS. C. ENGEBRETSON'S ADDITION PART OF LOT 1, MRS. C. ENGEBRETSON PART OF LOT 1, MRS. C. ENGEBRETSON PA PART OF OUTLOTS 36, 37, 38, 39 & 46, ASSESSOR'S PLAT, VILLAGE OF CASHTON; PART OF LOTS 2 & 3, CSM, V. 6, P 72, DOC. NO. 378197; AND PART OF LOTS 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18 & 19, BLOCK 1, SMITH'S ADDITION; ALL LOCATED IN THE SE 1/4 - SW 1/4, ALL IN SECTION 30, T15N, R3W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

EASEMENT TABLE

RECORDING INFORMATION

EASEMENT DOC. NO. 637710

NO EASEMENT OF RECORD FOUND

NO EASEMENT OF RECORD FOUND

CENTURYTEL OF MONROF

VERNON COMMUNICATIONS COOPERATIVE

PARCELS 5&6

S89°00'18"W 2672.58' SEC. COR. TO SEC. COR.

COUNTY, LLC

PARCELS 5&6

LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES. MONROF COUNTY.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY

NAD83 (1991) IN U.S SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID

BEARINGS AND DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

FOUND PROPERTY PIPES ARE 1" IRON PIPES UNLESS OTHERWISE NOTED.

OWNER

CENTURYTEL OF MONROE COUNTY, LLC

VERNON COMMUNICATIONS, LLC

WE ENERGIES - GAS

NOTES:

R=570.00' R=515.00' PC=79+79.26 PC=87+33.26 PT=82+15.77 PT=90+22.94 BK TAN=N66°52'45"W

PI=80+99.24

D=10°03'07"

T=119.981

L=236.51

BASIS

STH 33 PREVIOUS PROJECT 5143-01-21

LOCATED IN R/W PARCEL NO.

5-6

5.6

Y=305,244.368

X=632,948.240

DELTA=23°46'24" LT

FOUND HARRISON MON. = 310,599.661 X = 634,224.675

PI=88+82.05

D=11°07'31"

T=148.78

1 = 289.68

Y=305,253.326

X=633,734.452

DELTA=32°13'42" LT

19 30

PI=93+95.69

D=13°10'17"

T=87.97'

L=173.60

R=435.00'

PC=93+07.72

PT=94+81.32

Y=305,536.463

X=634,172,433

DELTA=22°51'55" LT

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT. TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

TO EFFECT THIS CHANGE, PURSUANT 10 AUTHORIT GRANTED GROVE GROUPS GROVE G

UTILITY INTERESTS REQUIRED

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY AS SHEET 2 OF 2 OF DOCUMENT NUMBER 652481 AND FILED IN MF295B.

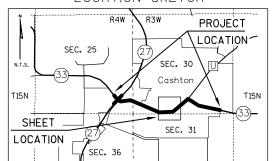
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
103	103 WE ENERGIES - GAS 104 VERNON COMMUNICATIONS COOPERATIVE	
104		
105 CENTURYTEL OF MONROE COUNTY, LLC		RELEASE OF RIGHTS

SEE SHEET 2 OF 2 FOR COURSE TABLES, OFFSET TABLES, CURVE INFORMATION AND DETAILS. NO0°21'35"W 62.89' R/L TO R/W 95+40.74. 0.00' RT VILLAGE Y=305,658.291 . 335(423) X=634.255.394 C. ENGEBRETSON'S - N34°15'13"F SUBDIVISION 300(422) MRS. C. ENGEBRETSON'S (45**`** N00°21'35"W 59.76' ADDITION R/W TO R/L SE-SW OF ASSESSOR'S PLAT V. 6, P. 72 ST 46 VILLAGE OF CASHTON DOC. #378197 CSM V. 13. P. 195 DOC. #491555 N00°21'35"W 321.32' (211)316 SEC. COR. TO R/W SCALE, FEET N84°20'50"E - LEWIS 0.L.31 STREET 0.L.30 CASHTON NE-NW LOT 1 ASSESSOR'S PLAT VERNON COMMUNICATIONS CSM FOUND P.K. NAIL 15 A VILLAGE OF CASHTON 31 = 305,277.216 COOPERATIVE V. 6, P. 73 PARCELS 5&6 DOC. #378198 FOUND HARRISON MON. WE ENERGIES - GAS

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES SCHEDULE OF LANDS & ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER INTERESTS REQUIRED OF LAND INTEREST TO D.O.T. ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

PARCEL			INTEREST	R/W ACRES REQUIRED			TLE
	NUMBER	OWNER(S)	REQUIRED	NEW	EXISTING	TOTAL	ACRES
	5	VILLAGE OF CASHTON	FEE,TLE	0.01		0.01	0.02
	6	CASHTON AREA SCHOOL DISTRICT	FEE,TLE	0.03		0.03	0.35
	7	REBECCA A. CLEMENTS	TLE				0.01
	9	LINUS J. SCHREJER	FEE,TLE	0.02		0.02	0.06
	35	HUNDT IMPLEMENT, LLC.	FEE	0.01		0.01	
	38	RICHARD E. EIRSCHELE	TLE				0.01
	39	CARMEN K. MLSNA	TLE				0.01
	40	LUCY & ERNEST A. SCHMITZ, LIFE ESTATE, ELIZABETH JEAN SCHMITZ, KENNETH LEE SCHUTTEMEIR, JUDITH ANN WILLIAMS & KEITH ELMER SCHUTTEMEIR	FEE,TLE	0.01		0.01	0.02
	41	RITA A. BYERS	FEE,TLE	0.01		0.01	0.01
	43	SHAUN M. MICKELSON	FEE,TLE	0.02		0.02	0.04
	44	CYNTHIA L. SOLTAU & MARK W. CONNELLY	TLE				0.04
	45	SPENCER W. & SANDRA RIPLEY	FEE,TLE	0.01		0.01	0.03
	46	LUCILLE H. DICKMAN, LIFE ESTATE, MARY JO E. MCDANIEL, DIANA J. THURSTON, STEPHEN C., JEROME R. & JOHN DICKMAN, KRISTI L. FISCHER	FEE,TLE	0.01		0.01	0.01
	47	MARTIN E. & ANGEL M. MYTAS	FEE,TLE	0.01		0.01	0.03
'	48	VICTOR A. GUTIERREZ & LINDA S. HORSWILL	TLE				0.01
	49	CHARLES H. & BETTY J. CUMMINGS	TLE				0.02
	50	BRYAN J. DEMPSEY & KIMBERLY A. MILLER	TLE				0.02
	51	BRIAN C. & JODY C. BRUEGGEN	TLE				0.02
	52	GARY K. LARSON	FEE,TLE	0.01		0.01	0.02

LOCATION SKETCH



Engineering

. THOMAS J. HANSEN. PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.03 AND THAT SUCH PLAT CORRECTL REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



Than I Han THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033

THIS PLAT AND RELOCATION ORDER ARE APPROVED. FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

Cory Schlage 10-29-15 DATE

PLOT DATE: 28/10/2015

PARCEL 9

6

PLOT NAME :

PLOT SCALE : *********

= 305,230.811

= 631,585,615

715 1

COUNTY, LLC PARCELS 5&6

CENTURYTEL OF MONROE

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.03

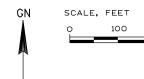
RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

	COURSE TABLE	
COURSE	BEAR I NG	DISTANCE
300-301	S41° 49' 46"W	17.90'
301-302	SEE CURVE I	DATA
302-303	S57° 37' 02"W	281.07'
303-304	\$32° 52' 52"E	37,04
304-305	S32° 20' 39"E	56.98'
305-306	S84° 04' 00"W	49.72'
306-307	N87° 32' 08"W	39.96'
307-308	N45° 10' 36"W	55.18'
308-309	SEE CURVE	DATA
309-310	S80° 45′ 18″W	5.91'
310-311	S89° 42′ 19"W	102.49'
311-336	S89° 22' 00"W	432.01
336-312	N89° 26' 10"W	25,69'
312-313	N86° 20' 46"W	39.56'
313-314	S86° 12′ 50"W	51.96'
314-315	SEE CURVE	DATA
315-316	N00° 31' 09"W	86,71'
316-317	S75° 06' 49"E	34.23'
317-318	S75° 32' 02"E	64.79'
318-319	S82° 16' 14"E	12.19'
319-320	S89° 14' 51"E	35,50'
320-321	S89° 11' 45"E	40.01'
321-322	N89° 25′ 55 " E	504.91'
322-323	N00° 06' 05"E	27.47'
323-324	S89° 37' 52"E	0.77'
324-325	S66° 44' 01"E	54.56'
325-326	N74° 54' 25"E	24,04'
326-327	N71° 31′ 55″E	105.47'
327-328	N66° 45′ 55"E	10.27'
328-329	N63° 51' 51"E	9.98'
329-330	N57° 21' 55"E	265.10
330-331	N55° 38' 33"E	64.65'
331-332	N55° 14' 27"E	60.86'
332-333	SEE CURVE	DATA
333-334	N34° 39' 19"E	21,73'
334-335	N34° 21′ 55"E	134,36'
335-300	S00° 21' 35"E	122.66'

STATION OFFSET TABLE		
POINT	STATION	OFFSET
300	94+91.56	33.95' R
301	94+74.36	31.65' R
302	93+18.30	31.83' R
303	90+38.02	29,25' R
304	90+38.02	66.29' R
305	90+37.48	123.27' R
306	89+98.05	101.46' R
307	89+68.94	80,89' R
308	89+52,52	28,78' R
309	87+46.60	28.09' R
310	87+41.05	28.85' R
311	86+38.99	28.15' R
312	81+82,92	28,45' R
313	81+45.26	29.06' R
314	80+97.45	40.43' R
315	80+77.55	40.03' R
316	80+55,20	43,79' L
317	80+92.25	43.03' L
318	81+61.51	36.04' L
319	81+74.49	35.28' L
320	82+12.35	35.80' L
321	82+52.56	34.79' L
322	87+59.16	33.44' L
323	87+61.13	60.85' L
324	87+62.01	60.80' L
325	88+14.03	33.47' L
326	88+39.73	35,15′ L
327	89+53.08	34.53' L
328	89+64.08	34.09' L
329	89+74.76	33.95' L
330	92+43.10	34,91' L
331	93+07.72	36.58' L
332	93+73.95	33.93' L
333	94+32.55	33.62' L
334	94+56.09	35,32' L
335	95+92,50	35,73' L
336	82+07.39	28.07' R

CURVE 301-302	CURVE 308-309	CURVE 314-315	CURVE 332-333
L=167.78'	L=217.33'	L=21.31'	L=54.30'
LC=166.51'	LC=215.85'	LC=21.31'	LC=54.01'
LCB=S45°23'21"W	LCB=S76°35'22"W	LCB=N76°41'41"W	LCB=N44°52'11"
R=393.10'	R=536.62'	R=1507.09'	R=150.00'

TLE STATION OFFSET TABLE			
POINT	STATION	OFFSET	
T350	80+90,49	50,42' L	
T351	81+73.93	42.01' L	
T352	82+06.90	43.32' L	
T353	82+06.77	51.40' L	
T354	82+12,27	51,45' L	
T355	82+52,58	51.40' L	
T356	82+57.59	51.45' L	
T357	82+57.69	40.79' L	
T358	84+05.63	40.57' L	
T359	84+05.64	44.57' L	
T360	85+57.68	44.34' L	
T361	85+57.67	40.34' L	
T362	87+54.17	39.73' L	
T363	87+55.47	61.16' L	
T364	88+14,56	38,54' L	
T365	89+41.90	40.67' L	
T366	89+40.16	50.86' L	
T367	89+69.37	47.69' L	
T370	92+34.19	49,92' L	
T371	92+34.16	44.92' L	
T372	93+93.01	42.49' L	
T373	94+31.97	38.59' L	
T374	95+04.96	40,66' L	
T375	95+05,00	45,66' L	
T376	96+05.87	44.95' L	
T377	94+84.20	39.02' R	
T378	93+31.02	38.00' R	
T379	93+30,74	43,00' R	
T380	93+05.74	43.00' R	
T381	93+05.79	38.00' R	
T382	90+38.02	38.00' R	
T383	90+34,93	127,55' R	
T384	89+83.51	99.40' R	
T385	88+77.97	93.29' R	
T386	88+77.97	38.61' R	
T387	86+10.64	38.00' R	
T388	86+10.45	68.00' R	
T389	85+29.50	68.00' R	
T390	85+29.64	38.00' R	
T391	81+83.12	38.98' R	
T392	81+44.03	39,08' R	
T393	87+36.81	38,01' R	
T394	88+38.96	40.13' L	



SHEET 2 OF 2 FOR COURSE TABLES, OFFSET TABLES, CURVE INFORMATION AND DETAILS.

TPP NUMBER 5100-08-22 - 4.03 SHEET 2 OF 2

FILE NAME : G:\WDOTSW\14023-004\CIVIL3D\SHEETSPLAN\RW\040101_RP.DWG

TLE 5

(211)(316

SLOPES

VILLAGE

52 TLE SLOPES

313/312 336 STH 33

TLE SLOPES

PLOT DATE: 12/14/2012 7:46 PM

SE-SW

SLOPES

FRONT STREET 311

NE-NW

T390/ TLE SLOPES

TLE 41

PLOT BY: SCOTT DEMEUSE

(335)423)

_*T377*

LEWIS STREET

CASHTON

PLOT NAME : _____

5100-08-22-4.03

SE-SW

(335)423

NOO°21'35"W 321.32' SEC. COR. TO R/W

30

31

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD83 (1991) IN U.S SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND DISTANCES, GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

¹⁹

30

VILLAGE

FOUND HARRISON MON.

CSM

V. 6, P. 104

DOC. #379381

0.L.54

RASIS

PREVIOUS PROJECT 5143-01-21

KISSEL'S ADDITION

KISSEL

SW-SE

45'

407

406 405

ASSESSOR'S PLAT

VILLAGE OF CASHTON

DOC. #477314

<u>10</u>

DOC. #578047

CASHTON

리

JOHNSON'S ADDITION

SEC. COR. TO SEC. COR.

= 310,599.661

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

FOUND PROPERTY PIPES ARE 1" IRON PIPES UNLESS OTHERWISE NOTED.

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.04, AMENDMENT NO. 1

THIS AMENDMENT ADDS PARCEL 72 TO TRANSPORTATION PROJECT PLAT 5100-08-22-4.04, RECORDED AS DOC. NO. 653052 AND FILED IN MAP FILE 297A IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY.

PART OF LOTS 1, 2, 3, 4, 5, 6 & 7, BLOCK 1, CASH'S ADDITION; PART OF LOT 2, CSM, V, 6, P, 71, DOC, NO, 378196; PART OF LOTS 1 & 2, CSM, V. 6, P. 72, DOC. NO. 378197; PART OF LOT 2, CSM, V. 6, P. 104, DOC. NO. 379381; PART OF LOT 1, CSM, V. 12, P. 169, DOC. NO. 477314; PART OF LOTS 1 & 2, KISSEL'S ADDITION; PART OF LOTS 15 & 16, JOHNSON'S ADDITION; AND PART OF OUTLOTS 46, 47, 48, 49, 50, 52, 53, 54, 55, 60, 61, 62, 63, 64 & 65, ASSESSOR'S PLAT, VILLAGE OF CASHTON, ALL LOCATED IN THE SW 1/4 - SE 1/4, SECTION 30, T15N, R3W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN.

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INITERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

PI=109+56.47

Y=305,622.244

X=635,355,962

D=11°27'33"

T=14.30'

L=28.60'

R=500-00

PC=109+42.17

PT=109+70.77

PI=111+93.28

D=11°14'04"

T=28.531

L=57.00'

R=510.00' PC=111+64.75 PT=112+21.75 AH TAN=S78°33'49"E

Y=305,601.473

X=635,591.866

DELTA=06°24'16" RT

GN

SCALE, FEET

100

DELTA=03°16'39" LT

PI=100+00.00

Y=306,037.891

X=634.513.890

PI=106+65.07

Y=305,664.818

X=635,064.462

D=13°58'29" T=93.951

L=184.71'

R=410.00'

ONTARIO

FOUND HARRISON MON.

Y = 305,255.996

X = 636.928.168

(500)400LOT 9

CSM

DOC. #578047

LOT 8 LOT 7

CSM

V. 21, P. 156

DOC. #578047

V. 21, P. 156 🖒

PC=105+71.12

PT=107+55.83

DELTA=89°52'06" RT

DELTA=25°48'45" LT

111+82,94, 0,00' RT Y=305,602.057 X=635,581,530

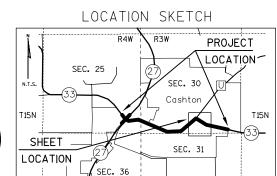
SEE SHEETS 2 & 3 OF 3 FOR COURSE TABLES. OFFSET TABLES, CURVE INFORMATION AND DETAILS.

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES

SCHEDULE OF LANDS & ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO D.O.T. INTERESTS REQUIRED ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED		R/W ACRES REQUIRED NEW EXISTING TOTAL		TLE ACRES
5	VILLAGE OF CASHTON	FEE.TLE	0.02		0.02	0.01
6	CASHTON AREA SCHOOL DISTRICT	FEE.TLE	0.05		0.05	0.02
11	CASHTON AREA DEVELOPMENT CORPORATION	FEE	0.15		0.15	
12	JEFFREY & JUDY WILLIAMS	FEE,TLE	0.01		0.01	0.01
13	BETHANY IMMANUEL, TRINITY LUTHERAN PARISH	FEE,TLE	0.01		0.01	0.01
14	SCOT A. WALL	TLE				0.01
15	JANICE M. WALL SURVIVOR'S TRUST DATED 7/1/1988	TLE				0.01
16	KAREN BRUEGGEN	FEE,TLE	0.01		0.01	0.02
17	DAVID H. & MARY JO KABAT	TLE				0.01
26	CASHTON VETERINARY REAL ESTATE, LLC	FEE,TLE	0.02		0.02	0.01
27	NATHAN F. EVERHART	FEE,TLE	0.01		0.01	0.01
28	JOHN P. FREDERICK	FEE,TLE	0.02		0.02	0.04
29	SUSAN A. TWAY	FEE,TLE	0.01		0.01	0.01
30	RODNEY MICKELSON	FEE,TLE	0.01		0.01	0.02
31	ROGER D. BERG	FEE,TLE	0.01		0.01	0.01
32	BJ ENTERPRISES OF ROCKLAND, LLC	FEE,TLE	0.01		0.01	0.02
33	PAUL W. & JEANINE A. VON RUDEN	FEE,TLE	0.01		0.01	0.02
34	HERBERT BOISEN	FEE,TLE	0.01		0.01	0.02
35	HUNDT IMPLEMENT, LLC.	FEE,TLE	0.02		0.02	0.02
36	VILLAGE OF CASHTON, ET AL	TLE				0.01
37	PHIL G. & PATRICIA C. MLSNA	TLE				0.02
38	RICHARD E. EIRSCHELE	TLE				0.02
67	JOINT SCHOOL DISTRICT NUMBER ONE OF THE TOWN OF PORTLAND & JEFFERSON MONROE COUNTY	FEE,TLE	0.01		0.01	0.01
70	HUNDT IMPLEMENT, LLC.	FEE	0.04		0.04	
71	HUNDT IMPLEMENT, LLC.	FEE	0.06		0.06	
72	HUNDT IMPLEMENT, LLC.	FEE,TLE	0.01		0.01	0.01

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY AS SHEET 2 OF 2 OF DOCUMENT NUMBER 652481 AND FILED IN MF295B.





. THOMAS J. HANSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.04 AMENDMENT NO. 1 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



Than I Ham THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033 THIS PLAT AND RELOCATION ORDER ARE APPROVED.

ACCEPTED FOR RECORDING AND

FILING IN

THE OFFICE OF THE REGISTER OF

DEEDS IN MONROE COUNTY.

WISCONSIN

AT 9:00 A.M. ON MAY 23, 2016 AS

DOCUMENT # 657214

AND FILED IN MAP FILE 301B

4

RESERVED FOR REGISTER OF DEEDS

PROJECT NUMBER 5100-08-22-4.04

SHEET 1 OF 3

AMENDMENT NO. 1

FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION 5-18-2016 CORY SCHLAGEL DATE

FILE NAME : G:\WDOTSW\14023-004\CIVIL3D\SHEETSPLAN\RW\040101_RP-4,04 AMEND 1.DWG

= 305,277.216

634,257,787

6

DOC. #378197

SEE DETAIL "A"

ON SHEET 2 OF 3

HIGHWAY

STH 33

70

PLOT DATE: 17/05/2016

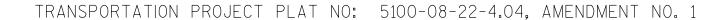
S89°32'41"E 2670.47'

PLOT BY : KL ENGINEERING

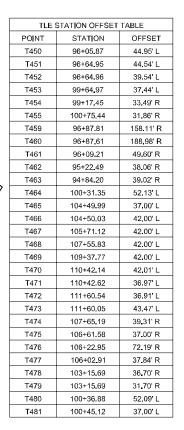
30 29 31 32

PLOT NAME :

PLOT SCALE : **********



RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY



STA	ATION OFFSET T	ABLE
POINT	STATION	OFFSET
400	111+95.63	31.15' R
401	110+07.56	35,44' R
402	107+65.82	34.35' R
403	106+59_87	32.17' R
404	106+66.04	50.12' R
405	106+29,76	67,60' R
406	106+26.63	69.68' R
407	106+05.45	33.04' R
408	100+68.94	31.87' R
409	99+22.18	30.92' R
410	96+82.83	160.81' F
411	96+83.02	189.02' F
412	96+40.53	190.09' F
413	96+33.29	190.14' F
414	96+33,23	182,27' F
415	96+25,79	170,57' F
416	96+22.31	170.75' F
417	96+22.20	169.91' F
418	96+09.73	120.84' F
419	96+09,66	110,80° F
420	96+21.84	109.69' F
421	96+21.41	51.22' R
422	94+91.56	33.95' R
423	95+92,50	35.73' L
424	99+64,98	35,00' L
425	100+00.00	46.64' L
426	100+31.22	32.00' L
427	105+71.12	32.00' L
428	107+55.83	32,00' L
429	109+37.75	32.00' L
430	109+73_14	32.00' L
431	111+65.86	31.91' L
432	111+64.96	43,91' L
433	111+67.56	44.14' L
440	100+31.32	47.13' L
441	100+33.90	47,11' L
442	100+42.15	32.00' L

COURSE TABLE					
COURSE	BEARING	DISTANCE			
400-401	N86° 01' 04"W	186-19'			
401-402	N82° 08' 05"W	243.70			
402-403	N74° 29' 05"W	113.43'			
403-404	S00° 33′ 23"W	19.19'			
404-405	N89° 26' 37"W	45.00'			
405-406	S86° 31′ 47"W	4.20			
406-407	N05° 12' 53"W	43,69'			
407-408	N55° 55' 05"W	539.24'			
408-409	S73° 48′ 54"W	59 <u>.</u> 81'			
409-410	S05° 45′ 55"W	272.33'			
410-411	S56° 08' 16"E	28,20'			
411-412	S32° 48′ 40"W	42.51'			
412-413	S33° 49′ 54 " W	7.24'			
413-414	N56° 10' 06"W	7.87'			
414 - 415	N88° 11' 22"W	13.87			
415-416	S31° 10′ 35″W	3.48'			
416-417	N62° 59' 32"W	0.85'			
417–418	N70° 00' 17"W	50.63'			
418-419	N56° 10' 03"W	10.04'			
419-420	N29° 03′ 02"E	12.23			
420-421	N56° 10' 03"W	58 <u>.</u> 47'			
421-422	S41° 49' 46"W	131.00'			
422-423	N00° 21' 35"W	122.66′			
423-424	N34° 21′ 55"E	372.48'			
424-425	N34° 21′ 55"E	66,01'			
425-426	S56° 41' 23"E	66.35'			
426-440	N34° 30′ 55"E	15.13'			
440-441	S55° 29' 05"E	2.58'			
441-442	S05° 29' 41"W	17.22'			
442 - 427	S55° 52' 41"E	528.96			
427-428	SEE CURV	E DATA			
428-429	S81° 41' 26"E	181_92'			
429-430	S83° 13' 46"E	33.55			
430-431	S84° 56' 26"E	192,79'			
431-432	N00° 30′ 46"E	12.04'			
432-433	S89° 31' 31"E	2.83'			
433-400	S13° 45' 54"E	80.45'			

4

CURVE 427-428

L=170.30' LC=168.86' LCB=S68°47'04"E R=378.00'

SHEET 2 OF 3 FOR COURSE TABLES, OFFSET TABLES, CURVE INFORMATION AND DETAILS.

TPP NUMBER 5100-08-22 - 4.04 SHEET 2 OF 3 AMENDMENT NO. 1

NOT TO SCALE

<u>CSM</u> V. 6, P. 71 DOC. #378196

KISSEL AVENUE

DETAIL "A"

0.L.55

ASSESSOR'S PLAT

VILLAGE OF CASHTON

49.5

(11)

<u>T459</u>

TLE SLOPES 67

0.L.51

0.L.52

ASSESSOR'S PLAT VILLAGE OF CASHTON

CSM .

V. 6, P. 72

DOC. #378197.

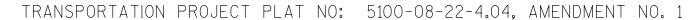
T461

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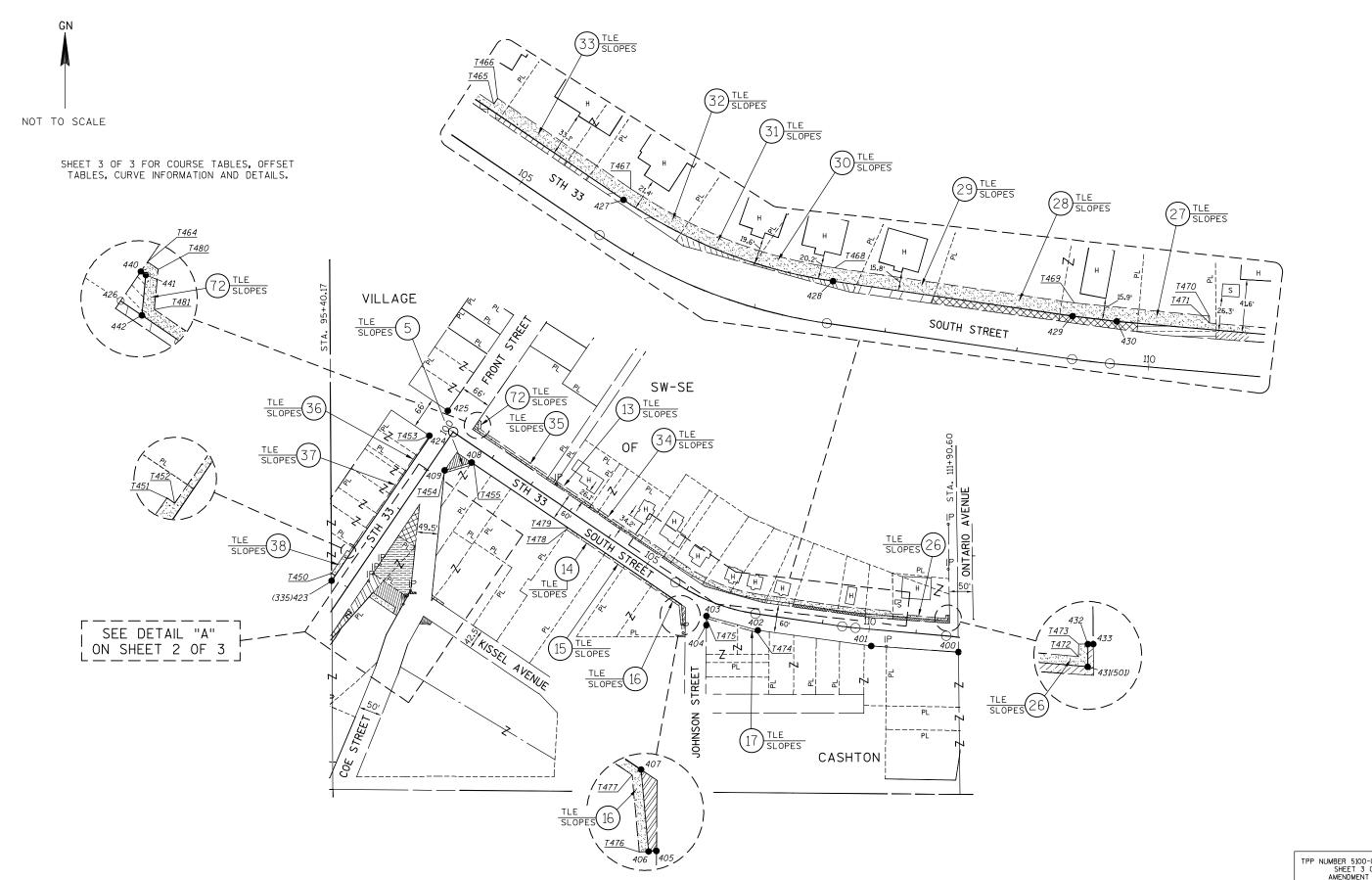
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TLE 6

0.L.56



RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY



TPP NUMBER 5100-08-22 - 4.04 SHEET 3 OF 3 AMENDMENT NO. 1

4

DOCUMENT # 653052 AND FILED IN MAP FILE 297A

RESERVED FOR REGISTER OF DEEDS SHEET 1 OF 3 PROJECT NUMBER 5100-08-22-4.04

4

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.04 PART OF LOTS 1, 2, 3, 4, 5, 6 & 7, BLOCK 1, CASH'S ADDITION; PART OF LOT 2, CSM, V. 6, P. 71, DOC. NO. 378196; PART OF LOTS 1 & 2, CSM, V. 6, P. 72, DOC. NO. 378197; PART OF LOT 2, CSM, V. 6, P. 104, DOC. NO. 379381; PART OF LOT 1, CSM, V. 12, P. 169, DOC. NO.

ADDITION; AND PART OF OUTLOTS 46, 47, 48, 49, 50, 52, 53, 54, 55, 60, 61, 62, 63, 64 &

65, ASSESSOR'S PLAT, VILLAGE OF CASHTON, ALL LOCATED IN THE SW 1/4 - SE 1/4, SECTION

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NADR3 (1991) IN ILS SURVEY FEET, VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND DISTANCES, GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

FOUND PROPERTY PIPES ARE I" IRON PIPES UNLESS OTHERWISE NOTED.

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

30, T15N, R3W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN.

477314; PART OF LOTS 1 & 2, KISSEL'S ADDITION; PART OF LOTS 15 & 16, JOHNSON'S

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

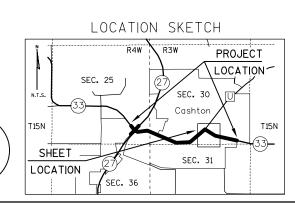
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (D) OR (2), WISCONSIN STATUTES.

HIGHWAY	BASIS
STH 33	PREVIOUS PROJECT 5143-01-21

SEE SHEETS 2 & 3 OF 3 FOR COURSE TABLES, OFFSET TABLES, CURVE INFORMATION AND DETAILS.

SCHEDULE OF LANDS &	OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER
INTERESTS REQUIRED	OF LAND INTEREST TO D.O.T.

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W .	ACRES REQ	UIRED TOTAL	TLE ACRES
5	VILLAGE OF CASHTON	FEE,TLE	0.02		0.02	0.01
6	CASHTON AREA SCHOOL DISTRICT	FEE,TLE	0.05		0.05	0.02
11	CASHTON AREA DEVELOPMENT CORPORATION	FEE	0.15		0.15	
12	JEFFREY & JUDY WILLIAMS	FEE,TLE	0.01		0.01	0.01
13	BETHANY IMMANUEL, TRINITY LUTHERAN PARISH	FEE,TLE	0.01		0.01	0.01
14	SCOT A. WALL	TLE				0.01
15	JANICE M. WALL SURVIVOR'S TRUST DATED 7/1/1988	TLE				0.01
16	KAREN BRUEGGEN	FEE,TLE	0.01		0.01	0.02
17	DAVID H. & MARY JO KABAT	TLE				0.01
26	CASHTON VETERINARY REAL ESTATE, LLC	FEE,TLE	0.02		0.02	0.01
27	NATHAN F. EVERHART	FEE,TLE	0.01		0.01	0.01
28	JOHN P. FREDERICK	FEE,TLE	0.02		0.02	0.04
29	SUSAN A. TWAY	FEE,TLE	0.01		0.01	0.01
30	RODNEY MICKELSON	FEE,TLE	0.01		0.01	0.02
31	ROGER D. BERG	FEE,TLE	0.01		0.01	0.01
32	BJ ENTERPRISES OF ROCKLAND, LLC	FEE,TLE	0.01		0.01	0.02
33	PAUL W. & JEANINE A. VON RUDEN	FEE,TLE	0.01		0.01	0.02
34	HERBERT BOISEN	FEE,TLE	0.01		0.01	0.02
35	HUNDT IMPLEMENT, LLC.	FEE,TLE	0.02		0.02	0.02
36	VILLAGE OF CASHTON, ET AL	TLE				0.01
37	PHIL G. & PATRICIA C. MLSNA	TLE				0.02
38	RICHARD E. EIRSCHELE	TLE				0.02
67	JOINT SCHOOL DISTRICT NUMBER ONE OF THE TOWN OF PORTLAND & JEFFERSON MONROE COUNTY	FEE,TLE	0.01		0.01	0.01
70	HUNDT IMPLEMENT, LLC.	FEE	0.04		0.04	
71	HUNDT IMPLEMENT, LLC.	FEE	0.06		0.06	





. THOMAS J. HANSEN. PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.04 AND THAT SUCH PLAT CORRECTL REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.



Than I Har THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033 THIS PLAT AND RELOCATION ORDER ARE APPROVED. FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

Cory Schlagel CORY SCHLAGEL DATE

FILE NAME : G:\WDOTSW\14023-004\CIVIL3D\SHEETSPLAN\RW\040101_RP.DWG

= 305,277.216

634,257,787

6

DOC. #378197

SEE DETAIL "A"

ON SHEET 2 OF 3

70

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY AS SHEET 2 OF 2 OF DOCUMENT NUMBER 652481 AND FILED IN MF295B.

30

P3 W

FOUND HARRISON MON. Y = 310,599,661 X = 634,224,675

VILLAGE

THOUS AND THE PROPERTY OF THE

CSM

V. 6, P. 104 DOC. #379381

0.L.54

KISSEL

0.L.52

200

SCALE, FEET

SE-SW

(335)423

NOO°21'35"W 321.32' SEC. COR. TO R/W

30

31

100

PLOT BY : KL ENGINEERING

30 29 31 32

PLOT NAME :

PLOT SCALE : ########

5100-08-22-4.04

PLOT DATE: 28/10/2015

S89°32'41"E 2670.47'

LOT 1

KISSEL'S ADDITION

45'

407

406 405

NOTES:

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE

PI=109+56-47 PI=111+93-28 PI=106+65.07 PI=100+00.00 Y=305,601.473 Y=305,622.244 Y=305,664.818 Y=306,037.891 X=635,355.962 X=635,591.866 X=634.513.890 X=635.064.462 DELTA=03°16'39" LT DELTA=25°48'45" LT DELTA=89°52'06" RT D=11°27'33" D=11°14'04" D=13°58'29" T=14.30' T=28.53' T=93.95' L=28.60' L=57.00' L=184.71 R=500-00 R=510.00' R=410.00' PC=109+42.17 PC=111+64.75 PC=105+71.12 PT=109+70.77 PT=112+21.75 PT=107+55.83

AH TAN=S78°33'49"E

SW-SE ASSESSOR'S PLAT VILLAGE OF CASHTON

V. 12, P. 169 DOC. #477314

<u>10</u>

V. 21, P. 156

DOC. #578047

CASHTON

리

JOHNSON'S ADDITION

SEC. COR. TO SEC. COR.

DELTA=06°24'16" RT

ONTARIO

(500)400 LOT 9

CSM

DOC. #578047

LOT 8

LOT 7

CSM

V. 21, P. 156

DOC. #578047

V. 21, P. 156

111+82,94, 0,00' RT Y=305,602.057 X=635,581,530

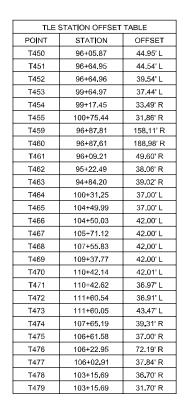
FOUND HARRISON MON.

Y = 305,255.996

X = 636.928.168



RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY



STATION OFFSET TABLE					
POINT	STATION	OFFSET			
400	111+95.63	31.15' R			
401	110+07.56	35.44' R			
402	107+65.82	34.35' R			
403	106+59.87	32.17' R			
404	106+66.04	50.12' R			
405	106+29.76	67.60' R			
406	106+26.63	69.68' R			
407	106+05.45	33.04' R			
408	100+68-94	31.87' R			
409	99+22.18	30.92' R			
410	96+82.83	160.81' R			
411	96+83.02	189.02' R			
412	96+40.53	190.09' R			
413	96+33.29	190.14' R			
414	96+33.23	182.27' R			
415	96+25.79	170.57' R			
416	96+22,31	170.75' R			
417	96+22.20	169.91' R			
418	96+09.73	120.84' R			
419	96+09.66	110.80' R			
420	96+21,84	109,69' R			
421	96+21.41	51.22' R			
422	94+91.56	33.95' R			
423	95+92.50	35.73' L			
424	99+64,98	35,00' L			
425	100+00.00	46.64' L			
426	100+31_22	32.00' L			
427	105+71.12	32.00' L			
428	107+55.83	32.00' L			
429	109+37.75	32.00' L			
430	109+73_14	32.00' L			
431	111+65_86	31.91' L			
432	111+64.96	43.91' L			
433	111+67,56	44,14' L			

	COURSE TABLE	
COURSE	BEARING	DISTANCE
400-401	N86° 01' 04"W	186.19'
401-402	N82° 08' 05"W	243.70'
402-403	N74° 29' 05"W	113.43'
403-404	S00° 33' 23"W	19.19'
404-405	N89° 26' 37"W	45.00'
405-406	S86° 31' 47"W	4.20'
406-407	N05° 12' 53"W	43,69'
407-408	N55° 55' 05"W	539.24'
408-409	S73° 48' 54"W	59.81'
409-410	S05° 45' 55"W	272,33'
410-411	S56° 08' 16"E	28,20'
411-412	S32° 48' 40"W	42.51'
412-413	S33° 49' 54"W	7.24'
413-414	N56° 10' 06"W	7.87'
414-415	N88° 11' 22"W	13,87'
415-416	S31° 10' 35"W	3,48'
416-417	N62° 59' 32"W	0.85'
417-418	N70° 00' 17"W	50.63'
418-419	N56° 10' 03"W	10.04'
419-420	N29° 03' 02"E	12.23'
420-421	N56° 10' 03"W	58.47'
421-422	S41° 49' 46"W	131.00'
422-423	N00° 21' 35"W	122.66'
423-424	N34° 21' 55"E	372.48'
424-425	N34° 21' 55"E	66,01'
425-426	S56° 41' 23"E	66.35'
426-427	S55° 52' 41"E	539.90'
427-428	SEE CURV	/E DATA
428-429	S81° 41' 26"E	181,92'
429-430	S83° 13' 46"E	33.55'
430-431	S84° 56' 26"E	192.79'
431-432	N00° 30' 46"E	12.04'
432-433	S89° 31' 31"E	2,83'
433-400	S13° 45' 54"E	80,45'
	•	

4

CURVE 427-428

L=170.30' LC=168.86' LCB=S68°47'04"E R=378.00'

SHEET 2 OF 3 FOR COURSE TABLES, OFFSET TABLES, CURVE INFORMATION AND DETAILS.

TPP NUMBER 5100-08-22 - 4.04 SHEET 2 OF 3

T463

NOT TO SCALE

4

<u>CSM</u> V. 6, P. 71 DOC. #378196

12 TLE SLOPES

KISSEL AVENUE

DETAIL "A"

0.L.55

ASSESSOR'S PLAT

VILLAGE OF CASHTON

49.5

(11)

<u>T459</u>

10

(<u>414</u>)

0.L.52

ASSESSOR'S PLAT VILLAGE OF CASHTON

TLE SLOPES 67

0.L.51

411

CSM .

V. 6, P. 72 DOC. #378197

421

T461

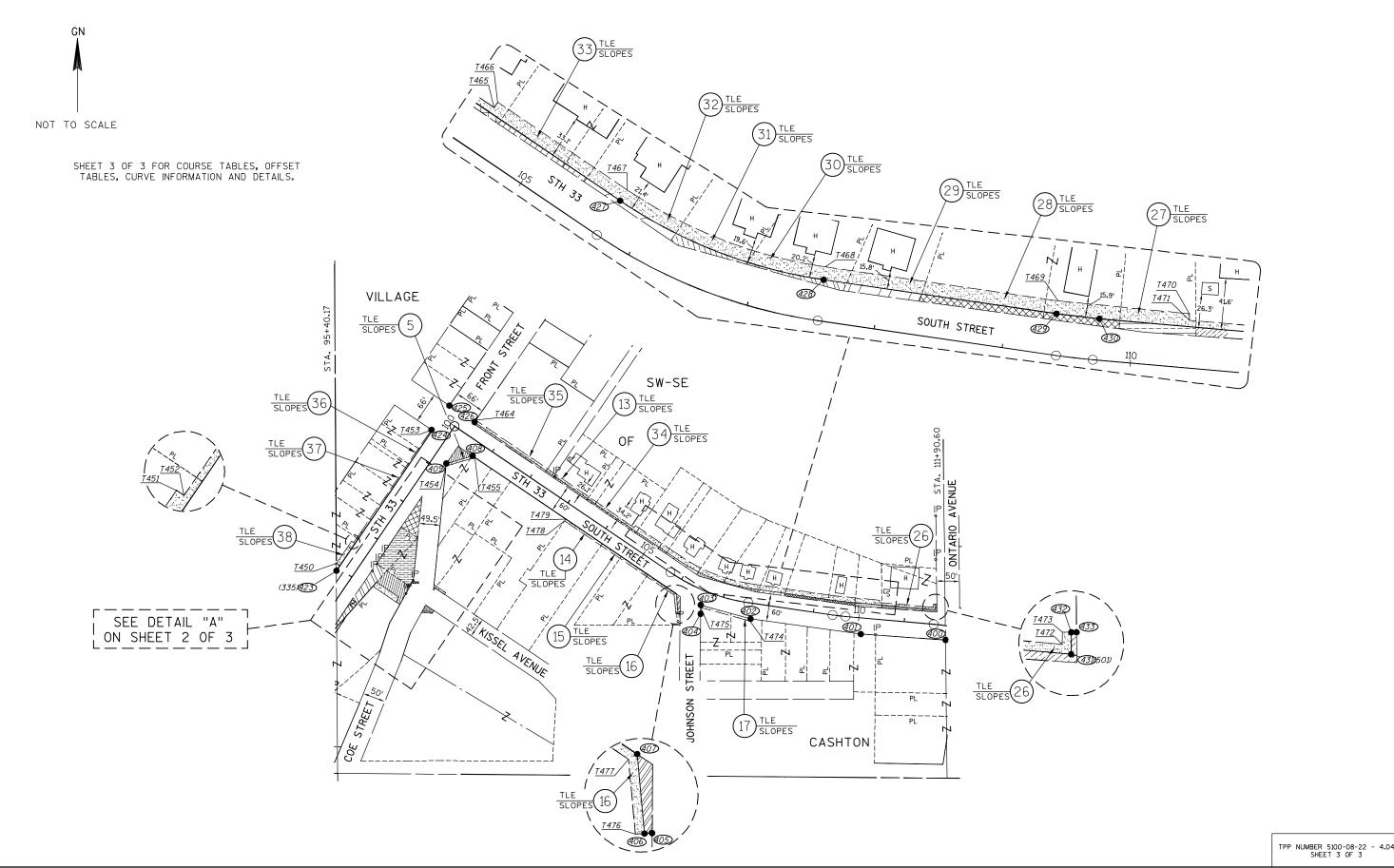
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TLE 6

0.L.\ 47 0.L.56

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY



4

AFFIDAVIT #6582

4

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD83 (1991) IN U.S SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

FOUND PROPERTY PIPES ARE 3/4" IRON PIPES UNLESS OTHERWISE NOTED.

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.05 AMENDMENT NO. 1

THIS AMENDS PARCEL 65 OF TRANSPORTATION PROJECT PLAT 5100-08-22-4.05, RECORDED AS DOC. NO. 653207 AND FILED IN MAP FILE 297B IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY.

PART OF LOT 4, KAMPERSHROER'S ADDITION; PART OF LOT 9, CSM, V. 21, P. 156, DOC. NO. 578047; PART OF LOT 1, CSM, V. 25, P. 97, DOC. NO. 626737; ALL LOCATED IN THE SE 1/4 - SE 1/4, SECTION 30, T15N, R3W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN.

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF

TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (D) OR (2), WISCONSIN STATUTES.

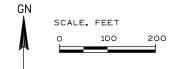
TLE STATION OFFSET TABLE POINT STATION OFFSET T550 112+18.09 55.62' R T551 115+35.00 41.46' L T552 115+35.00 51.17' L T553 115+42,35 52,39' L

STATION OFFSET TABLE						
POINT	STATION	OFFSET				
500	111+95.63	31.15' R				
501	111+65.86	31.91' L				
502	112+14.34	42.93' L				
503	115+44.18	41.42' L				
504	115+80,67	41.44' L				
505	116+91.21	41.38' L				
506	116+91.13	41.88' R				
507	116+36.15	51.86' R				
508	114+68.66	51 . 74' R				
509	114+68.66	40.75' R				
510	112+56.50	39.82' R				
511	112+13.13	29.56' R				

ACCEPTED FOR RECORDING AND FILING IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COLINTY WISCONSIN

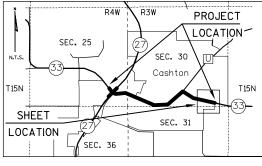
RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 5100-08-22-4.05 AMENDMENT NO. 1

4



STATION OFFSET TABLE						
POINT	STATION	OFFSET				
500	111+95.63	31.15' R				
501	111+65.86	31.91' L				
502	112+14.34	42.93' L				
503	115+44.18	41.42' L				
504	115+80,67	41.44' L				
505	116+91.21	41.38' L				
506	116+91.13	41.88' R				
507	116+36.15	51.86' R				
508	114+68.66	51.74' R				
509	114+68.66	40.75' R				
510	112+56.50	39.82' R				
511	112+13.13	29.56' R				

LOCATION SKETCH





THOMAS J. HANSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.05 AMENDMENT NO. 1 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

CORY SCHLAGEL



Than I Home 2/2/2016 THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033 THIS PLAT AND RELOCATION ORDER ARE APPROVED.

FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION Cory Schlagel 2/2/2016

HIGHWAY	BASIS			COURSE TABLE	
STH 33	PREVIOUS PROJECT'S 5143-01-21, 5100-07-21		COURSE	BEARING	DISTANCE
L			500-501	N18° 27' 47"W	69.71'
	0		501-502	N85° 56' 09"E	53.17'
	0.0	7553 TIF	502-503	S78° 18' 48"E	330.46
	الم	$\frac{7552}{1000}$ (23) TLE SLOPES	503-504	S78° 34' 57"E	36,50'
	4°.04 1111+9		504-505	S78° 31' 59"E	110.53'
	u de la company	T551/	505-506	S11° 29' 07"W	83.26'
	A α ις νισιασε ν	503	506-507	N88° 50' 46"W	55.88'
	_		507-508	N78° 31' 26"W	167,49'
	SIXTEENTH LINE 5100-08-2 LINE 5100-0	1 15 W	508-509	N11° 26' 11"E	10.99'
	되었다. ASSESSOR'S	SPLAT VALUEDOURDES SE-SE	509-510	N78° 18' 48"W	212.17'
SW-SE	N VILLAGE OF	CASHTON	510-511	N65° 11' 27"W	44.06'
	□ -	ADDITION	511-500	N86° 01' 07"W	16,54'
N00*40'44"W 302.95' N00*40'44"W 302.95' SEC. LINE TO R/W 100 100	(400)500 511 510 82.5° \$\frac{510}{578\sigma_33}\$	S89°32'41"E 2670.47	CORRECTED V		
7 15 N	\ \	N89°32'36"W 2670.34" S89°32'41"E 2670.47' SEC. COR. TO SEC. COR. 30 29	EOLIND HADD	USON MON	
((30)	FOUND P.K. NAIL	30 29	FOUND HARR Y = 305,255		
31 P 3 W	Y = 305,277.216 X = 634,257.787	CORRECTED VIA AFFIDAVIT #658222	X = 636,928		

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES

ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND INTEREST TO D.O.T.

R/W ACRES REQUIRED

EXISTING TOTAL

0.02

TLE

0.01

0.01

ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

NEW

0.02

INTEREST

REQUIRED

TLE

TLE

FEE

JEAN GILLES

SCHEDULE OF LANDS &

INTERESTS REQUIRED

DOUBLE D IMPROVEMENTS, LLC

OWNER(S)

TERRI S. KRAABEL N/K/A TERRI S. KEEPERS

PARCEL

NUMBER

21

23

65

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY AS SHEET 2 OF 2 OF DOCUMENT NUMBER 652481 AND FILED IN MF295B.

DATE

HIGHWAY

STH 33

30

31

AFFIDAVIT #65822

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD83 (1991) IN U.S SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND DISTANCES, GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS ARE TYPE 2 (TYPICALLY 3/4" X 24" REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

VILLAGE

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, SOUTHWEST REGION LACROSSE OFFICE.

PREVIOUS PROJECT'S 5143-01-21, 5100-07-21

FOUND PROPERTY PIPES ARE 3/4" IRON PIPES UNLESS OTHERWISE NOTED.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY AS SHEET 2 OF 2 OF DOCUMENT NUMBER 652481 AND FILED IN MF295B.

PI=111+93.28 Y=305,601,473 X=635,591.866 DELTA=06°24'16" RT D=11°14'04" T=28.531 L=57.00' R=510-00 PC=111+64.75 PT=112+21.75 BK TAN=N84°58'05"W

TRANSPORTATION PROJECT PLAT NO: 5100-08-22-4.05

PART OF LOT 4, KAMPERSHROER'S ADDITION; PART OF LOT 9, CSM, V. 21, P. 156, DOC. NO. 578047; PART OF LOT 1, CSM, V. 25, P. 97, DOC. NO. 626737; ALL LOCATED IN THE SE 1/4 - SE 1/4, SECTION 30, T15N, R3W, VILLAGE OF CASHTON, MONROE COUNTY, WISCONSIN.

RELOCATION ORDER STH 33 CASHTON - HILLSBORO, STH 27 TO V CASHTON EVL, MONROE COUNTY

COURSE TABLE

BEARING

N18° 27' 47"W

N85° 56' 09"E

S78° 18' 48"E

S78° 34' 57"F

S78° 31' 59"E

S11° 29' 07"W

N88° 50' 46"W

N78° 31' 26"W

COURSE

500-501

501-502

502-503

503-504

504-505

505-506

506-507

507-508

30 29 31 32

FOUND HARRISON MON.

r = 305,255.996

= 636,928.168

PLOT BY : KL ENGINEERING

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF

TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

209.17

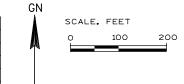
44.06'

16.54'

ACCEPTED FOR RECORDING AND FILING IN THE OFFICE OF THE REGISTER OF DEEDS IN MONROE COUNTY, WISCONSIN AT 9:33 A.M. ON NOV. 13, 2015 AS

DOCUMENT # 653207 AND FILED IN MAP FILE 297B

RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 5100-08-22-4-05



	TLE STATION OFFSET				
DISTANCE	POINT	STATION			
69,71'	T550	112+18.09			
53.17'	T551	115+35.00			
330.46'	T552	115+35,00			
36.50'	T553 115+42.35				
110.53					
83.26'					
55.88'	STATION OFFSET T				
170.49	POINT	STATION	L		
11,00'	500	111+95.63	L		

STATION OFFSET TABLE		
POINT	STATION	OFFSET
500	111+95.63	31.15' R
501	111+65.86	31.91' L
502	112+14.34	42.93′ L
503	115+44.18	41.42' L
504	115+80.67	41,44' L
505	116+91.21	41.38′ L
506	116+91.13	41,88' R
507	116+36.15	51.86' R
508	114+65.66	51.74' R
509	114+65.66	40.73' R
510	112+56.50	39,82' R
511	112+13,13	29,56' R

TABLE

OFFSET

55 62' R

41.46' L

51,17' L

52.39' L

508-509 N11° 26' 11"E 509-510 N78° 18' 48"W AVENUE 510-511 N65° 11' 27"W 511-500 N86° 01' 07"W ASSESSOR'S PLAT SE-SE KAMPERSHROER'S SW-SE FOUND HARRISON MON. VILLAGE OF CASHTON 30 29 IN CONCRETE **ADDITION** ARIO = 307,951,634 CASHTON TREE X = 636.904.859CASHTON BLOCK 2 킮 FFIDAVIT #6582 _7_ 0.L.19 502 님 111+79.90, 0.00' RT (431**/5**01**)** Ы Y=305,602,242 OF TOWN X=635,578.517 82.5 (400)500 116+91.17. 0.00' RT S78°33'49"E Y=305.502.739 SLOPES X=636,079.930 STH 33 508 CSM V. 25, P. 97 606 CSM SOUTH STREET DOC. #626737 CHETOOKA CIR. DOC. #578047 ONTARIO 40'44"W CASHTON S89°32'41"E 2670.47' N89°32'38"W 1335.12'

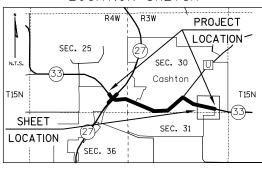
S89°32'41"E 2670.47'

TLE

0.01

0.01

LOCATION SKETCH



Engineering

. THOMAS J. HANSEN. PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT, I HAVE SURVEYED AND MAPPED TRANSPORTATION PROJECT PLAT 5100-08-22 - 4.05 AND THAT SUCH PLAT CORRECTL REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

CORY SCHLAGEL



Than I Ham 10-13-2015 THOMAS J. HANSEN AGENT FOR KL ENGINEERING, INC. P.I.S. NUMBER 2033 THIS PLAT AND RELOCATION ORDER ARE APPROVED. FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION

10/27/2015

SEC. COR. TO SEC. COR

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES

ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

NEW

0.02

INTEREST

REQUIRED

TLE

TLE

FEE

ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER

R/W ACRES REQUIRED

EXISTING TOTAL

0.02

PLOT DATE: 13/10/2015

SCHEDULE OF LANDS &

INTERESTS REQUIRED

JEAN GILLES

DOUBLE D IMPROVEMENTS, LLC

OWNER(S)

TERRI S. KRAABEL N/K/A TERRI S. KEEPERS

FOUND P.K. NAIL = 305,277.216 = 634.257.787

PARCEL

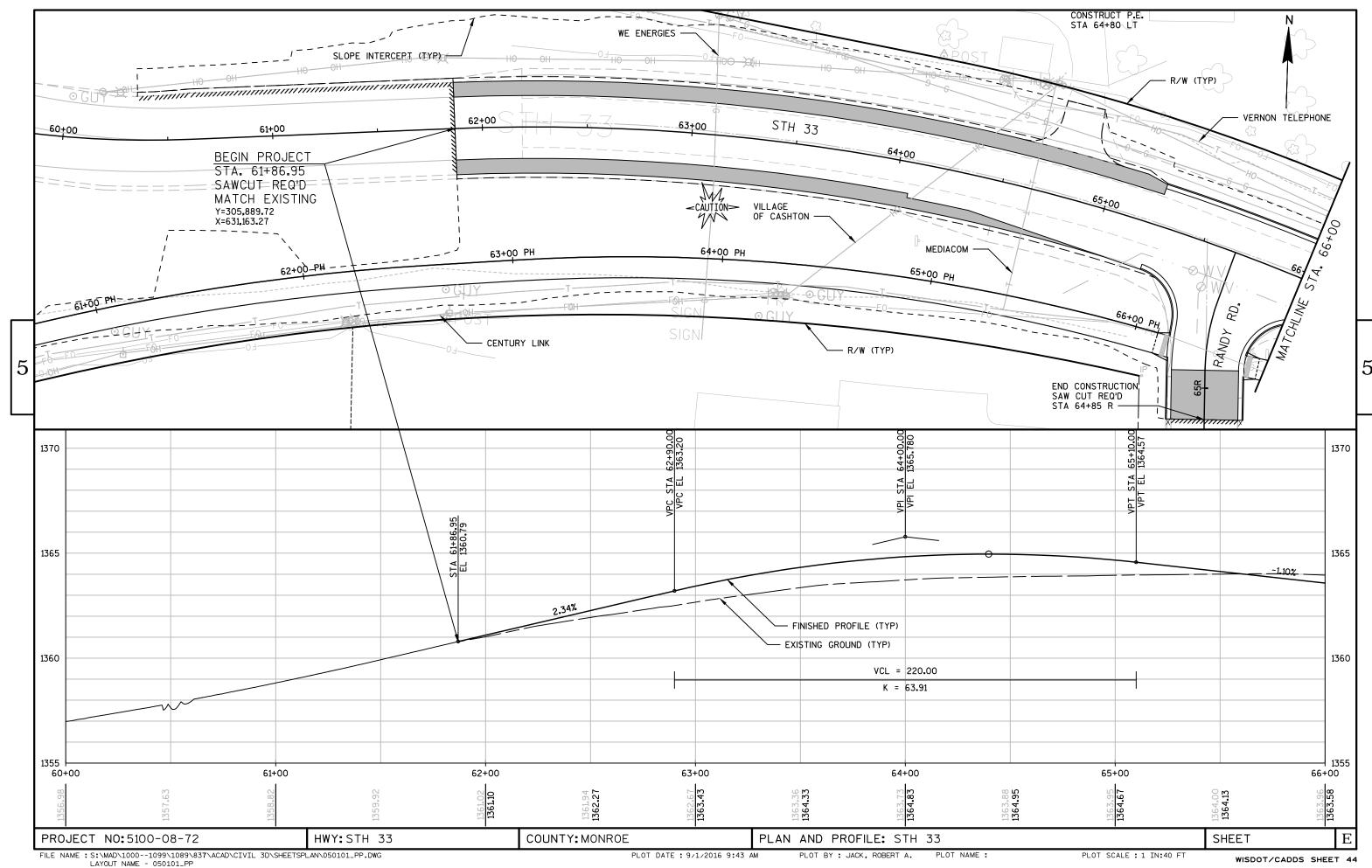
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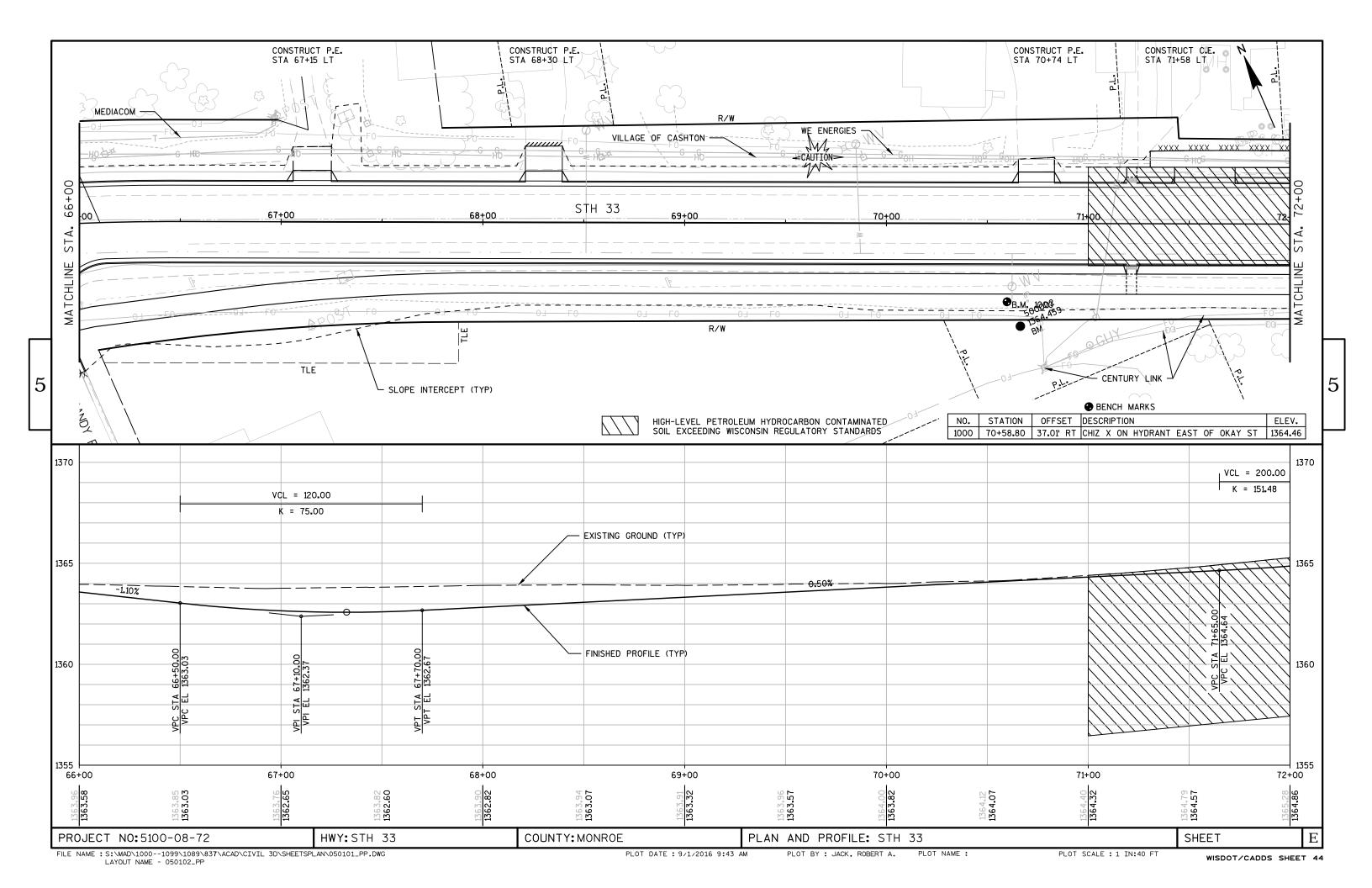
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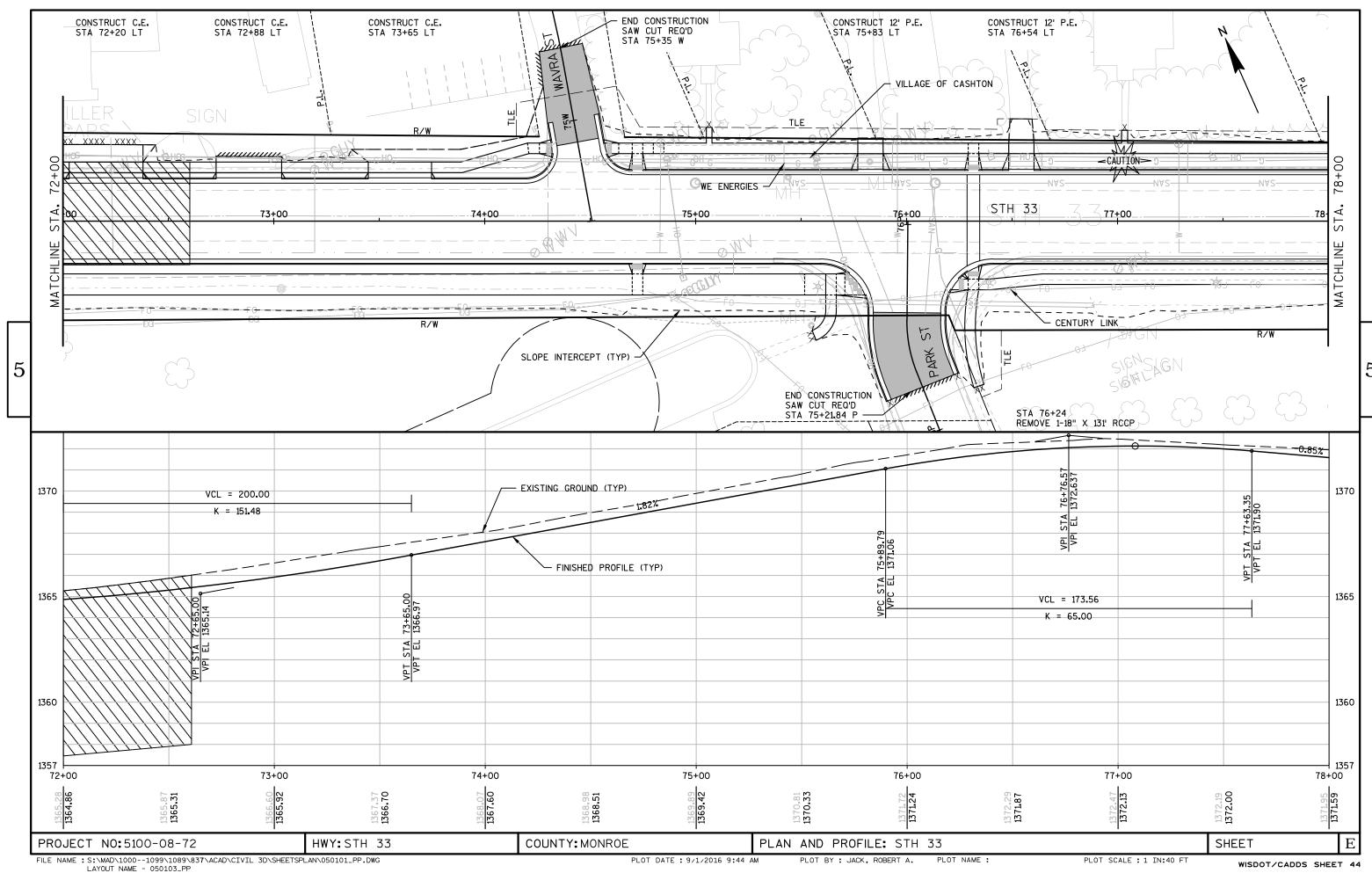
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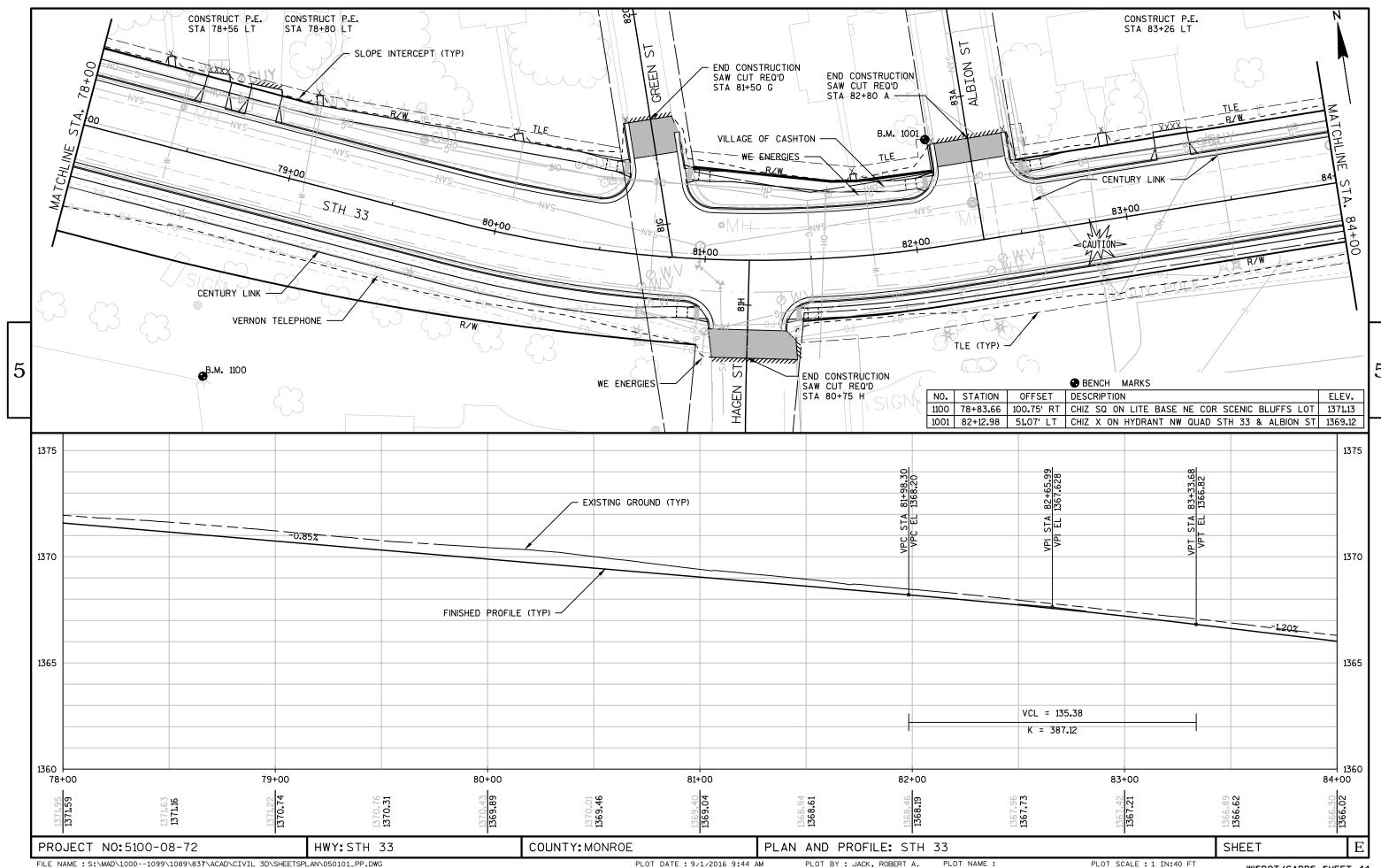
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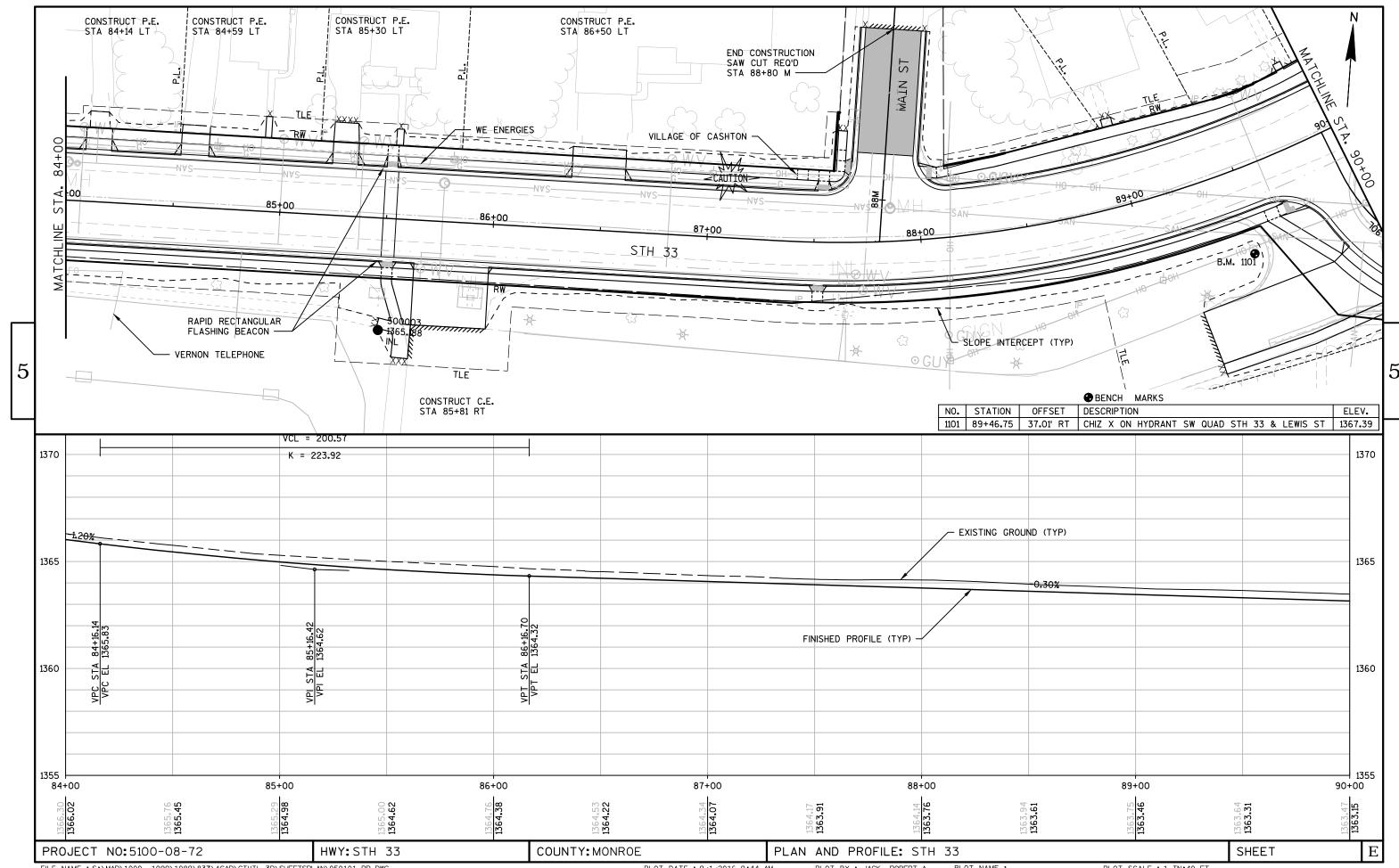
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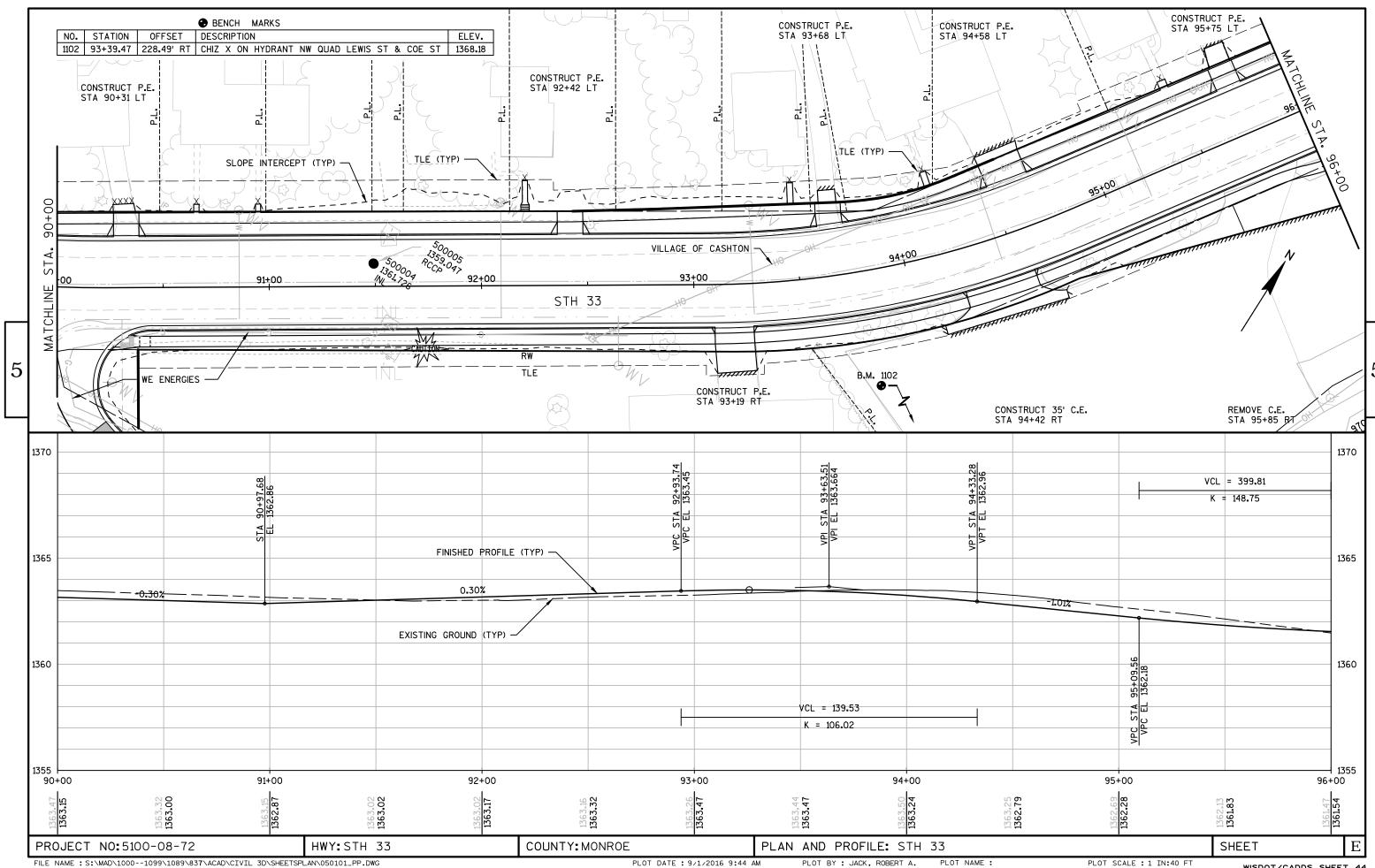


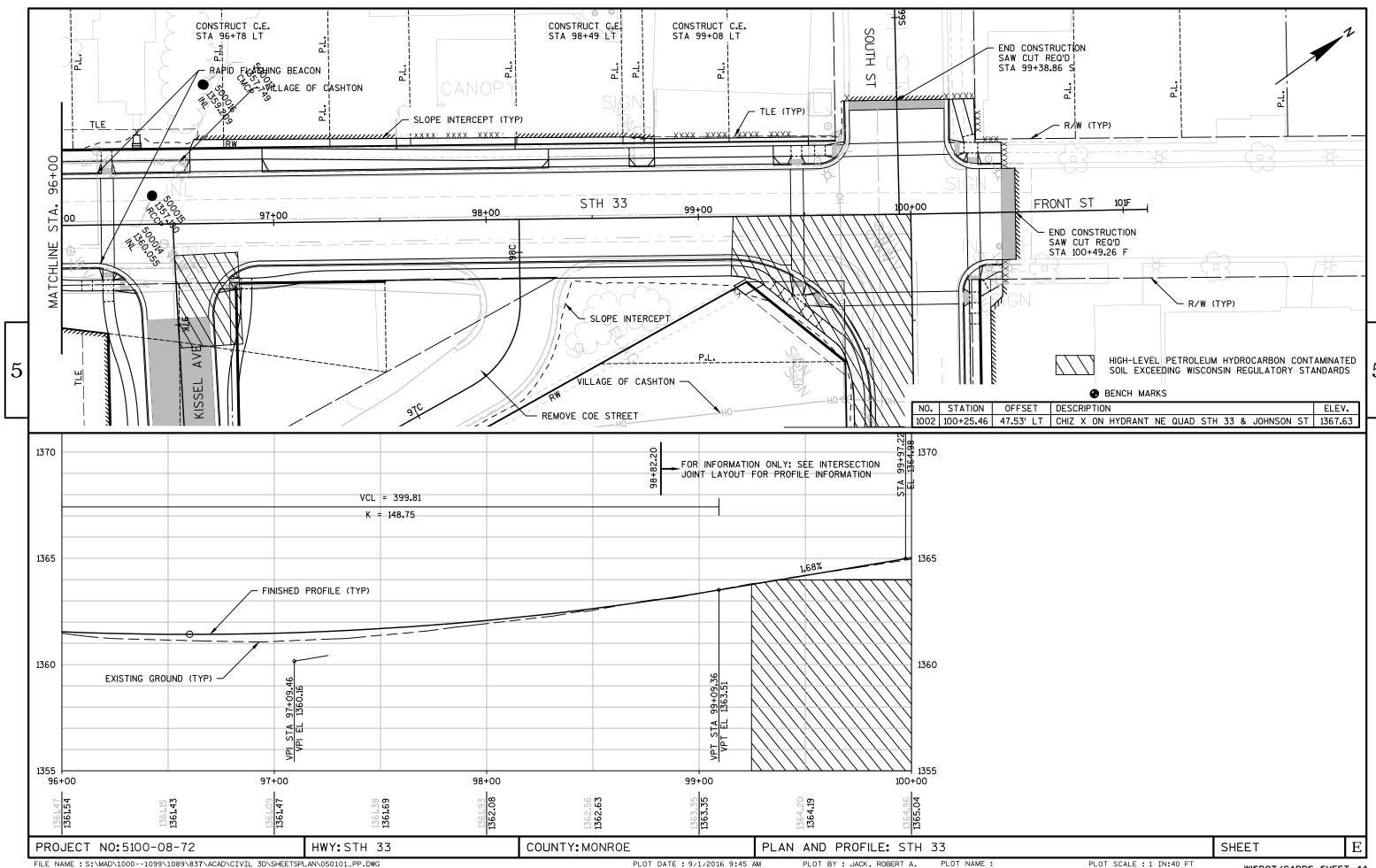


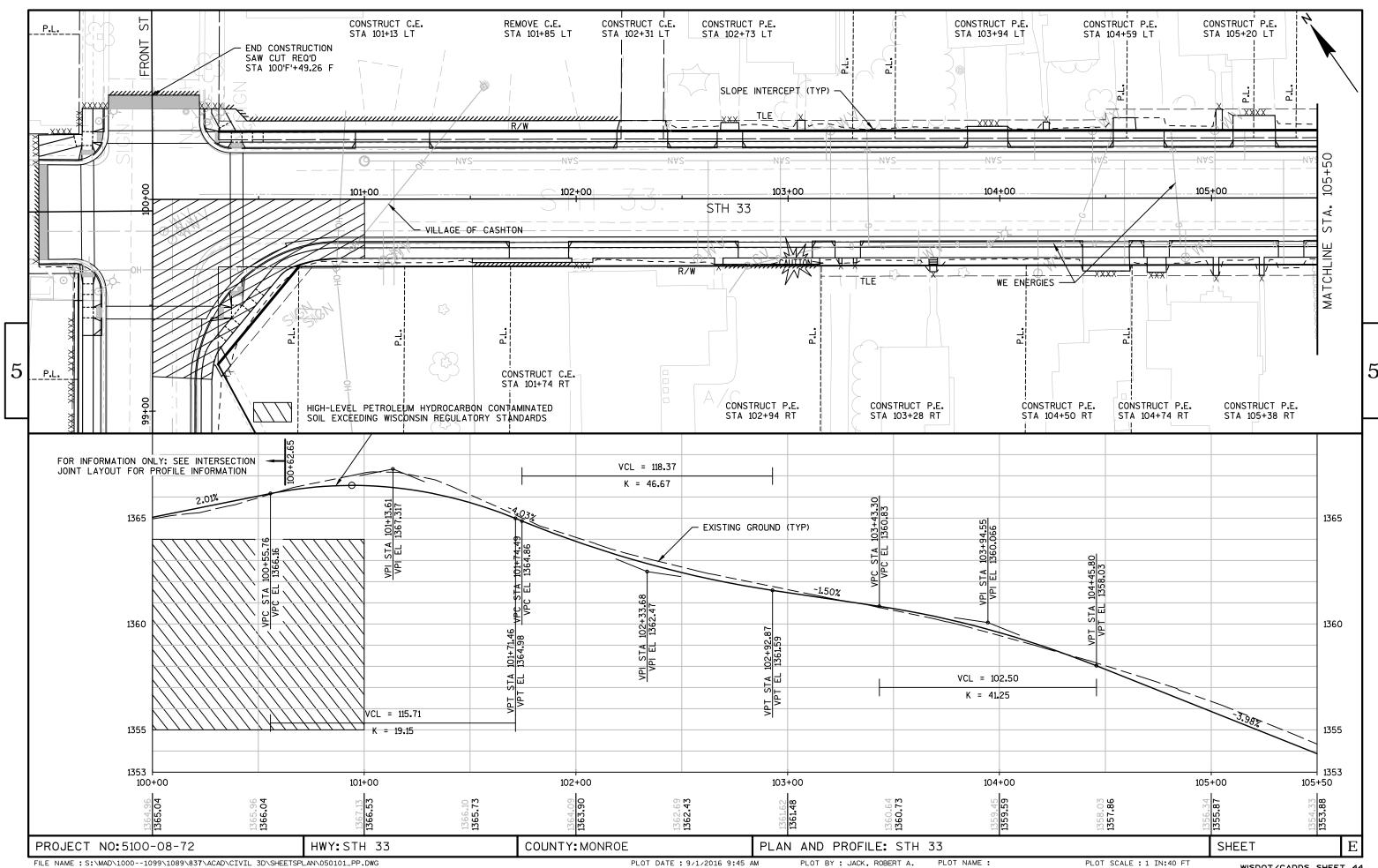


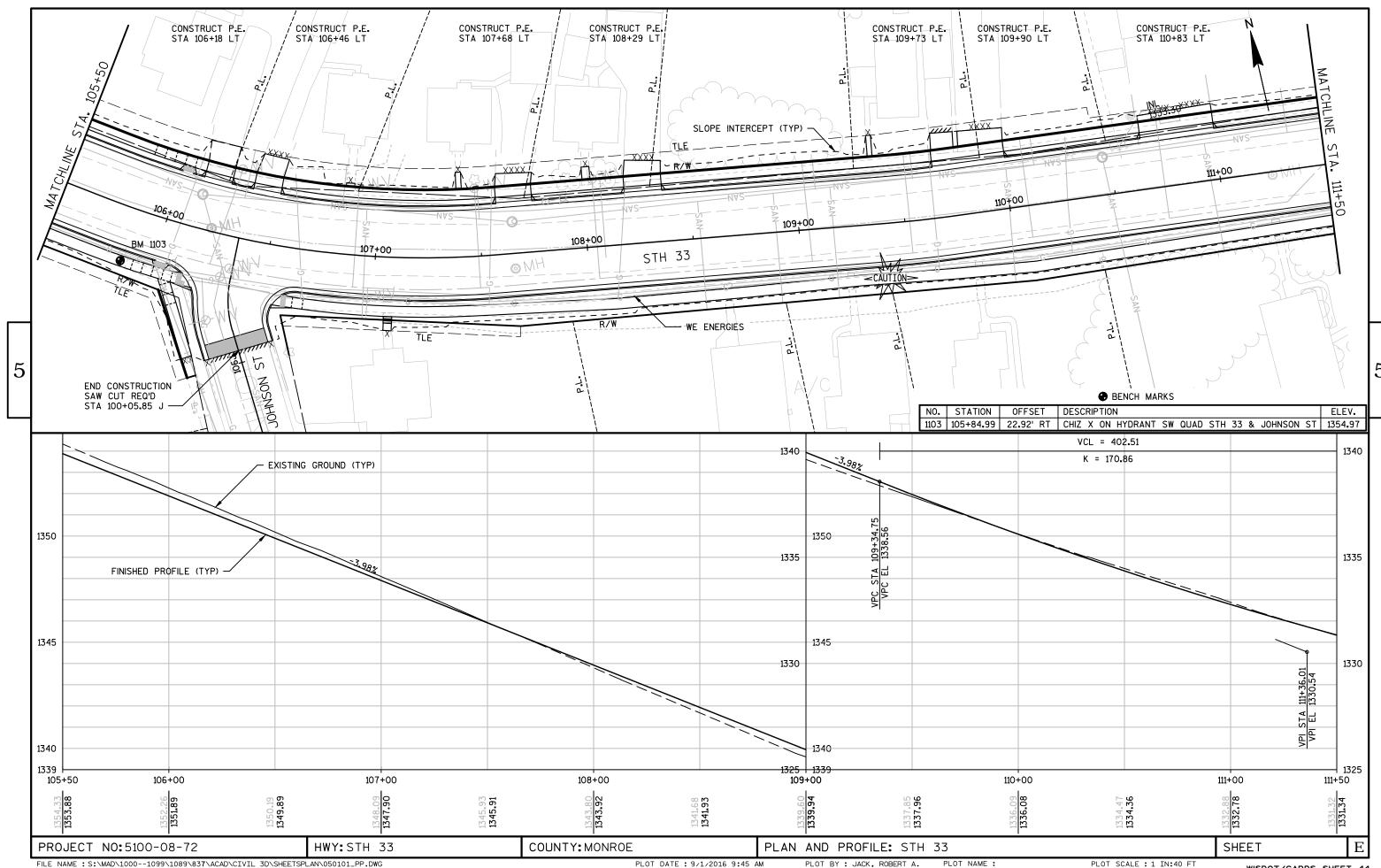


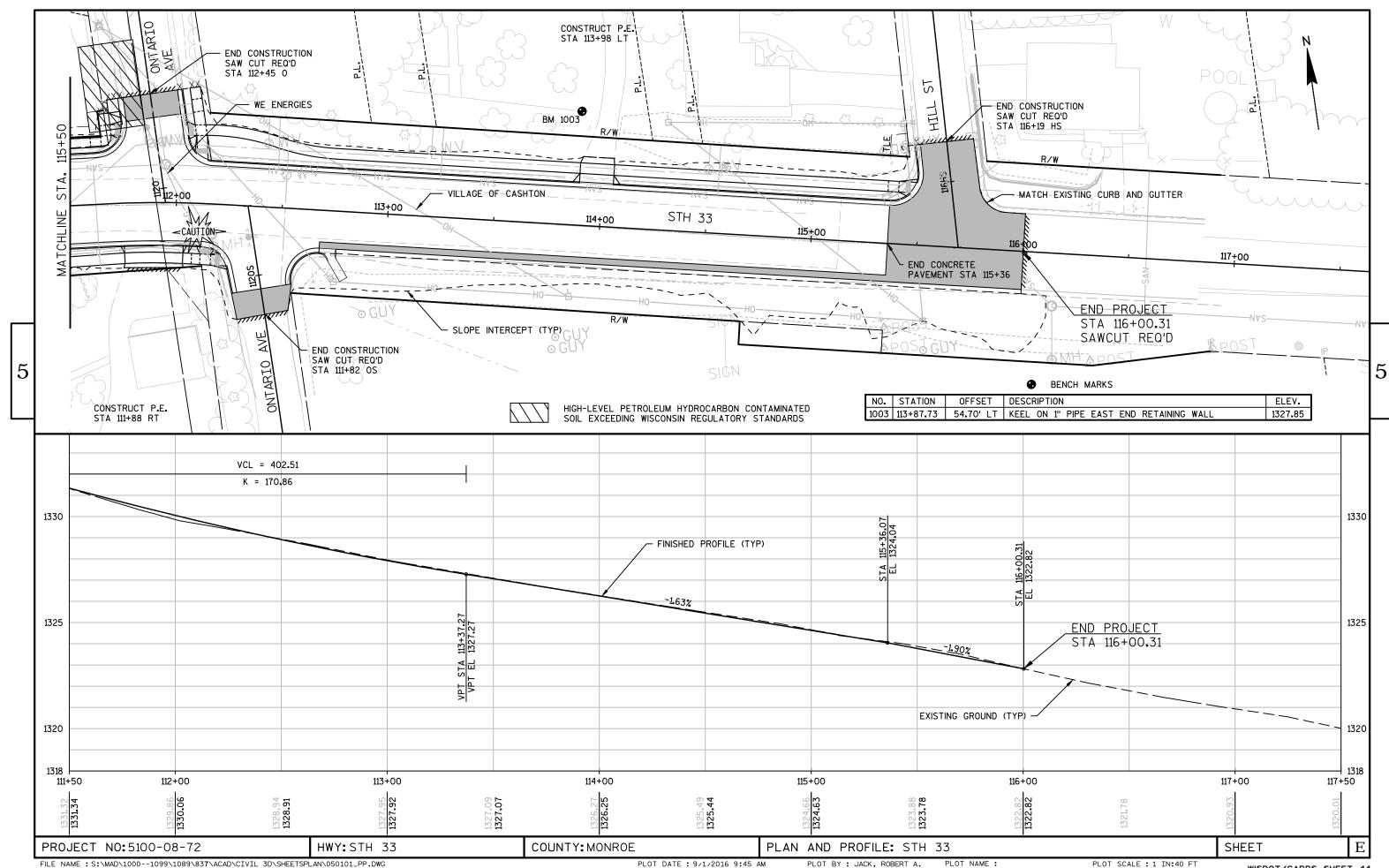




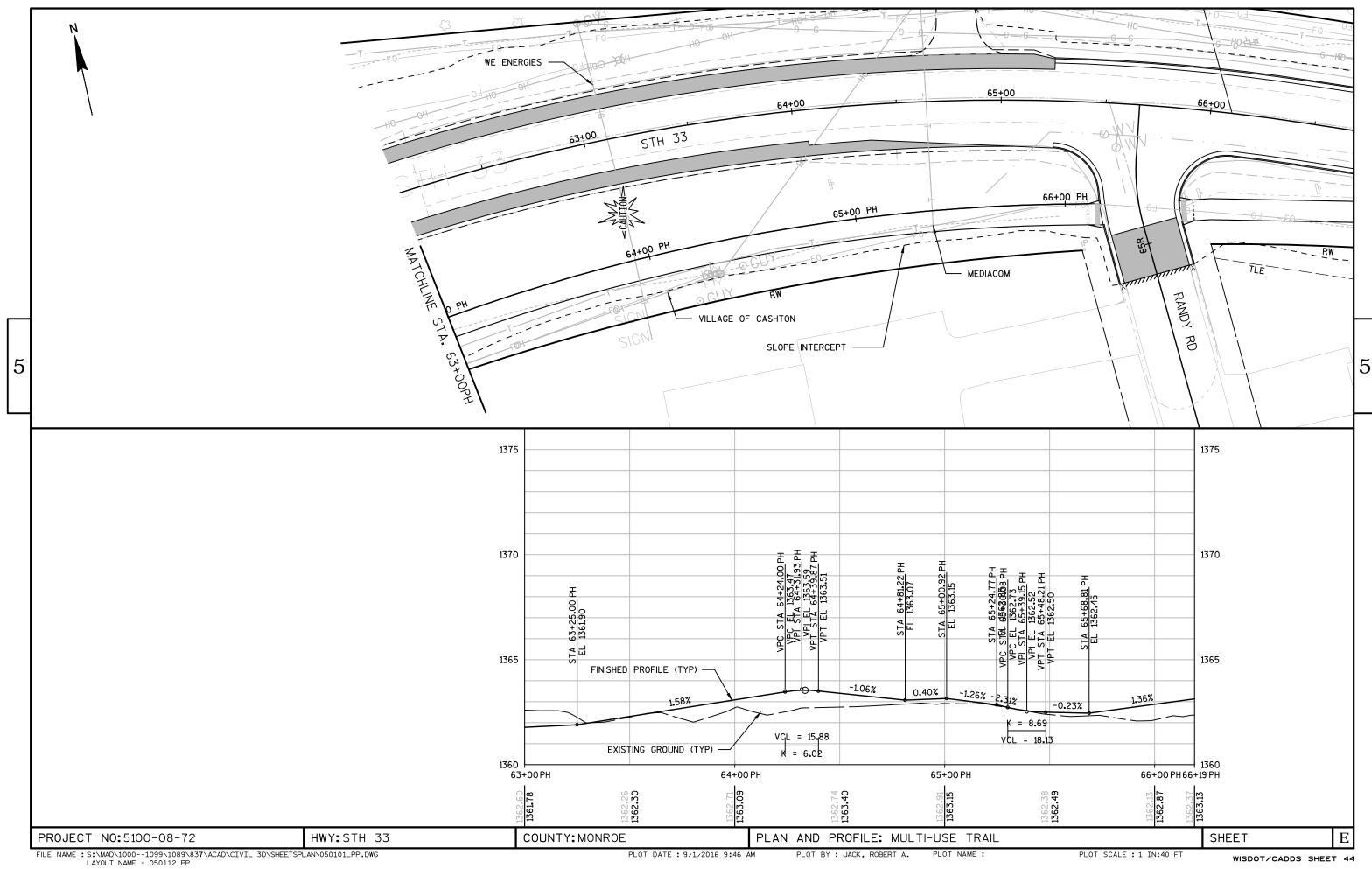


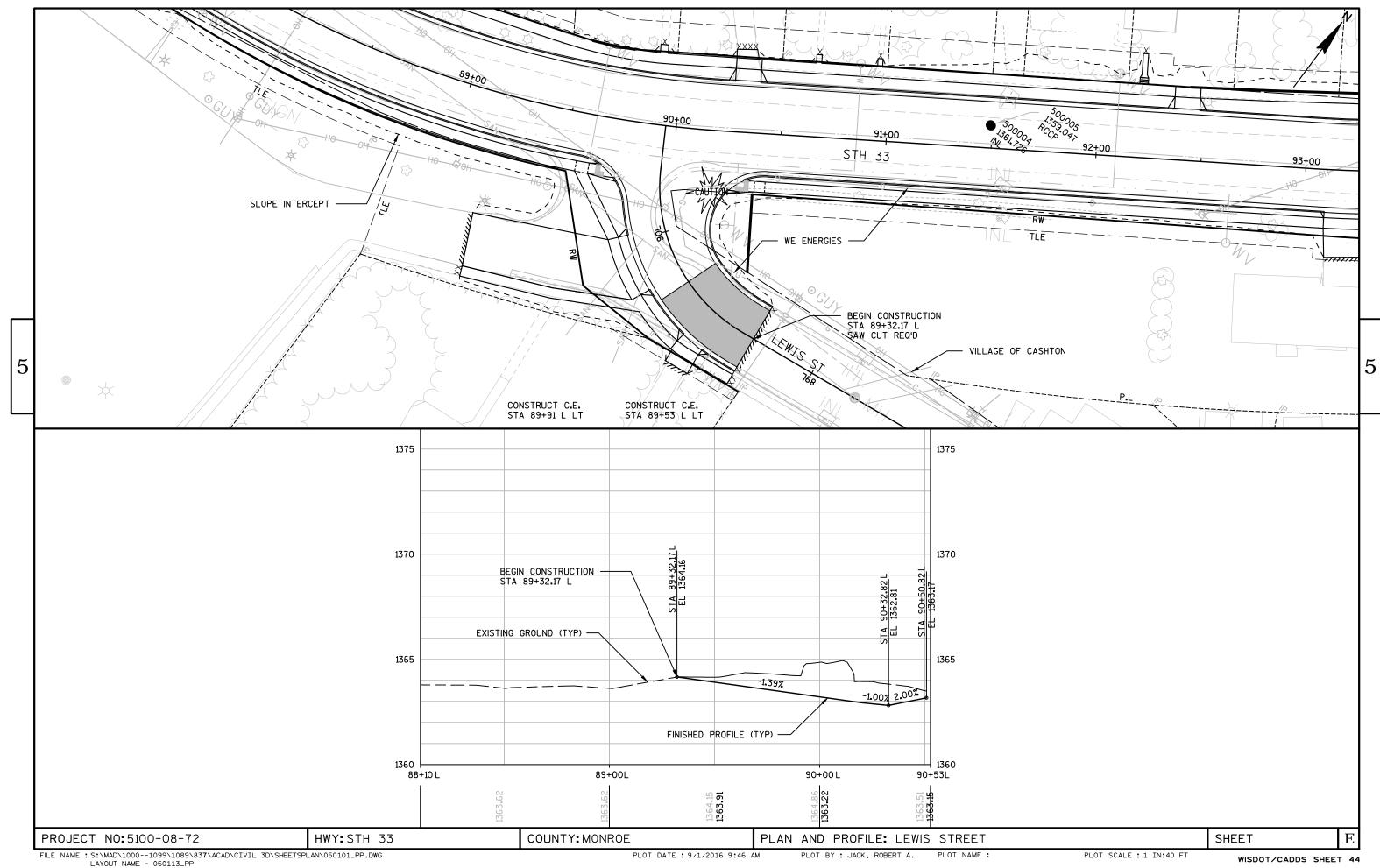


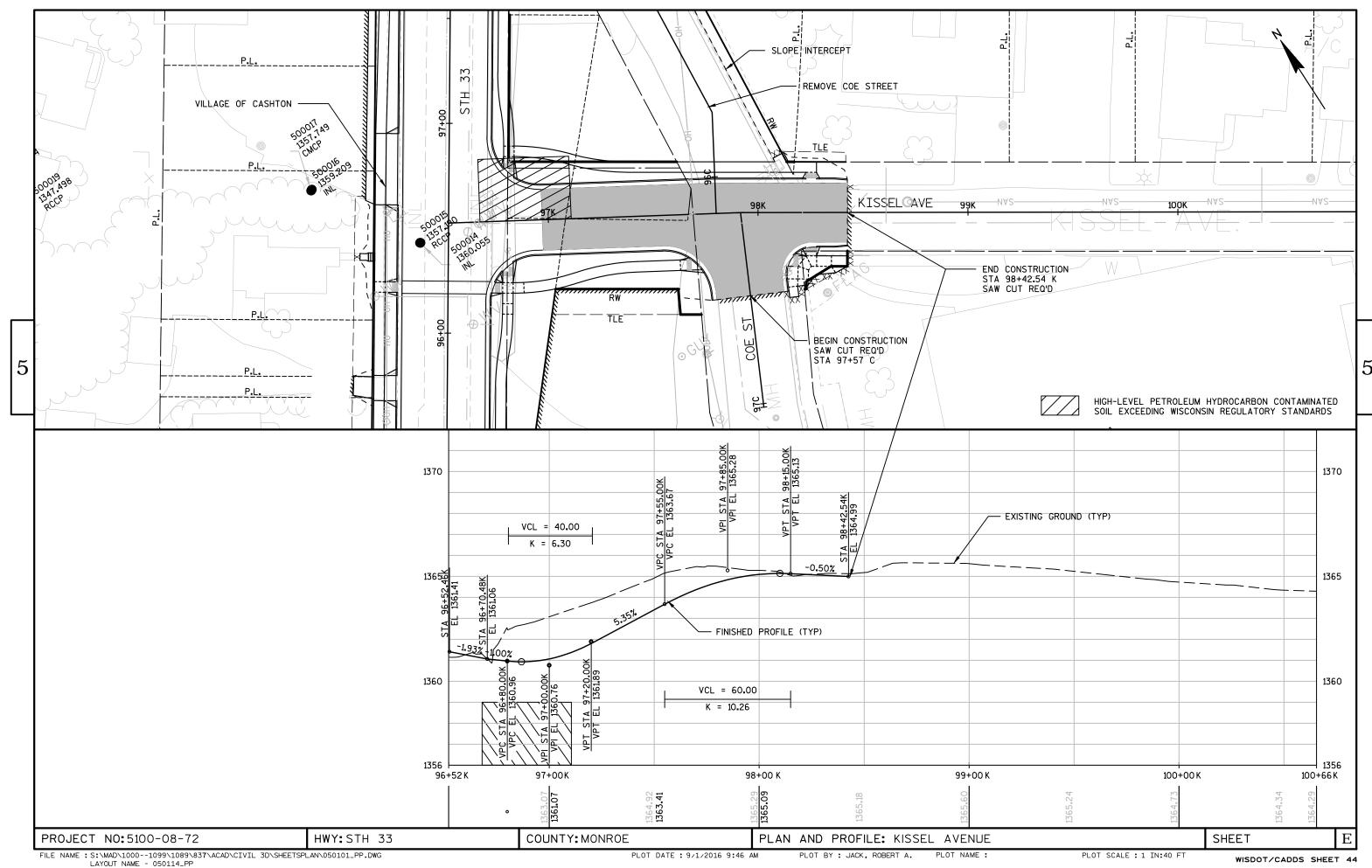








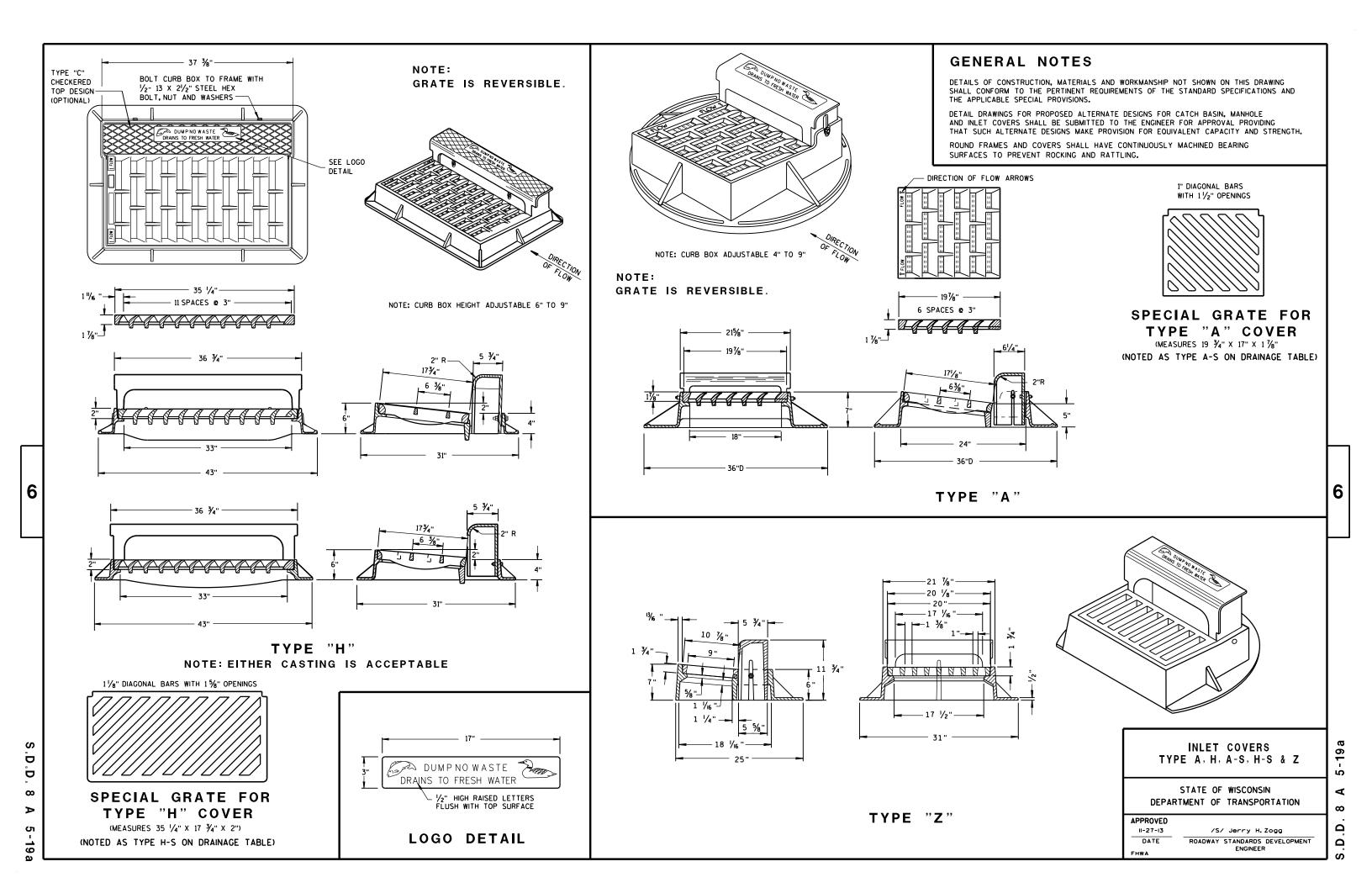


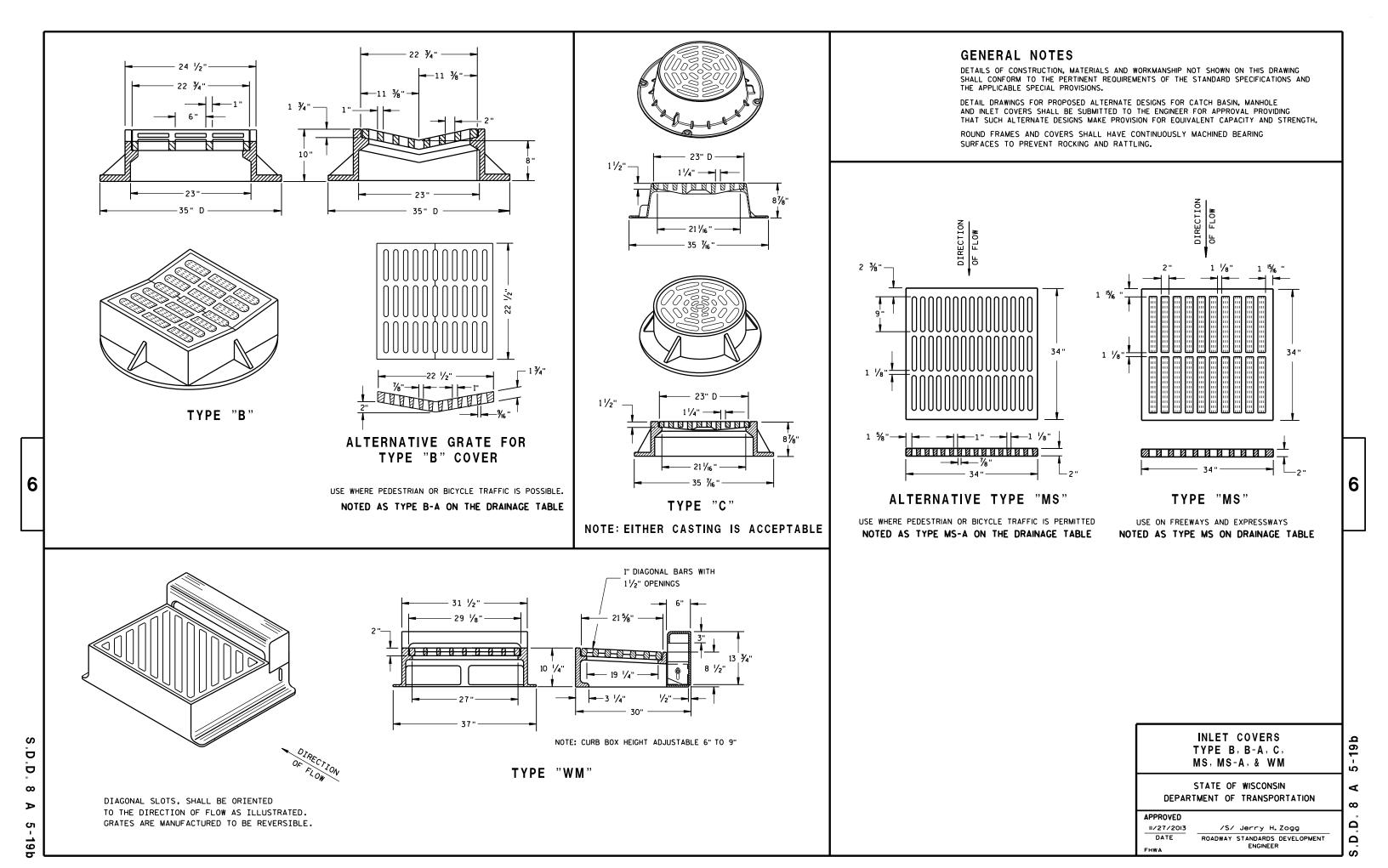


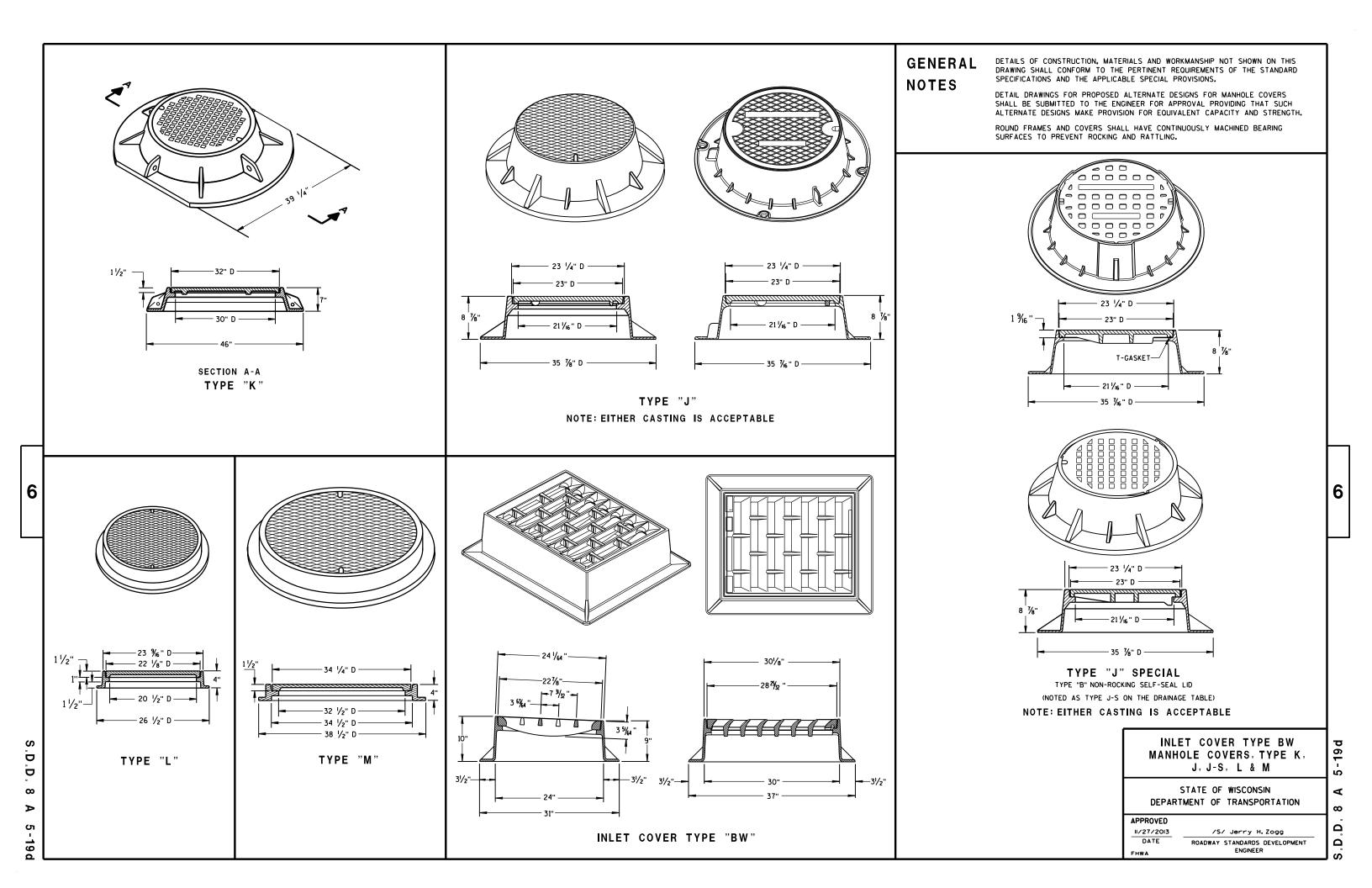
6

Standard Detail Drawing List

O9C12-08A CONCRETE BASE TYPE 13 O9C12-08B CONCRETE BASE TYPE 13 O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 12O/24O VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT	O9C12-08A CONCRETE BASE TYPE 13 O9C12-08B CONCRETE BASE TYPE 13 O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 120/240 VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES	O9C12-08A CONCRETE BASE TYPE 13 O9C12-08B CONCRETE BASE TYPE 13 O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 12O/24O VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT 13C18-03C CONCRETE PAVEMENT JOINT TIES	O9C12-08A O9C12-08B CONCRETE BASE TYPE 13 O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 120/240 VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT 13C18-03C CONCRETE PAVEMENT JOINT TIES 13C18-03D CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES 15C03-03 BARRICADES AND SIGNS FOR SIDEROAD CLOSURES 15C05-03 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS	08A05-19A 08A05-19B 08A05-19D 08B09-01 08C06-01 08C07-01 08C08-01 08D01-19 08D05-17A 08D05-17B 08D05-17C 08D05-17D 08D05-17E 08D15-05B 08D15-05C 08E08-03 08E09-06 08E10-02 08F01-11 08F04-07 09B02-09 09B04-11	INLET COVERS TYPE A, H, A-S, H-S & Z INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER INLETS 3-FT AND 4-FT DIAMETER INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT INLETS MEDIAN 1 AND 2 GRATE CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES CURB RAMPS TYPES 1 AND 1-A CURB RAMPS TYPES 2 AND 3 CURB RAMPS TYPES 4A AND 4A1 CURB RAMPS TYPES 4B AND 4B1 CURB RAMPS TYPES 5, 6, 7A, 7B & 8 EDGEDRAIN AND BASE AGGREGATE OPEN GRADED EDGEDRAIN AND BASE AGGREGATE OPEN GRADED TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS SILT FENCE INLET PROTECTION TYPE A, B, C AND D APRON ENDWALLS FOR CULVERT PIPE JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL CONDUIT PULL BOX
O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 120/240 VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT	O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 120/240 VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT 13C18-03C CONCRETE PAVEMENT JOINT TIES	O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 120/240 VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT 13C18-03C CONCRETE PAVEMENT JOINT TIES 13C18-03D CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES 15C03-03 BARRICADES AND SIGNS FOR SIDEROAD CLOSURES 15C05-03 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS	O9C13-02 CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION O9D01-05 CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL) O9D04-02 LIGHTING CONTROL CABINET 120/240 VOLT O9E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 O9E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT 13C18-03C CONCRETE PAVEMENT JOINT TIES 13C18-03D CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES 15C03-03 BARRICADES AND SIGNS FOR SIDEROAD CLOSURES 15C05-03 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS 15C08-16A PAVEMENT MARKING (MAINLINE) 15C33-02 STOP LINE AND CROSSWALK PAVEMENT MARKING 15D30-03A TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION		
09E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 09E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING 09E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT	09E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 09E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING 09E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT CONCRETE PAVEMENT JOINT TIES	09E01-14A POLE MOUNTINGS FOR TRAFFIC SIGNALS TYPE 2 09E03-05 NON-FREEWAY LIGHTING UNIT POLE WIRING 09E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT 13C18-03C CONCRETE PAVEMENT JOINT TIES 13C18-03D CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES 15C03-03 BARRICADES AND SIGNS FOR SIDEROAD CLOSURES 15C05-03 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS	O9E01-14A O9E03-05 O9E06-05 TRAFFIC SIGNAL STANDARD POLY BRACKET MOUNTINGS (TYPICAL) 13 FT. OR 15 FT. 13C01-18 CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES 13C13-08 URBAN DOWELED CONCRETE PAVEMENT 13C18-03A CONCRETE PAVEMENT JOINTING 13C18-03B CONCRETE PAVEMENT STEEL REINFORCEMENT 13C18-03C CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES 15C03-03 BARRICADES AND SIGNS FOR SIDEROAD CLOSURES 15C05-03 TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS 15C03-03 STOP LINE AND CROSSWALK PAVEMENT MARKING 15D30-03A TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	09C13-02	CONCRETE BASE TYPE 10 & TYPE 13 EXTENSION CABINET SERVICE INSTALLATION (METER BREAKER PEDESTAL)
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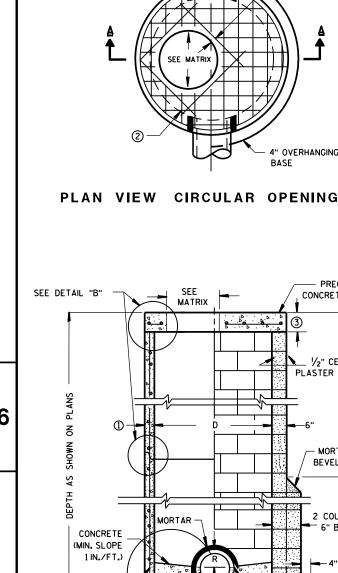






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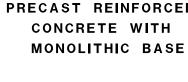
SEE

MORTAR -

MATRIX

• 4° • •

PRECAST REINFORCED — CONCRETE FLAT SLAB TOP



②-

CONTRACTOR TO PROVIDE DRAWING(S)

STAMPED BY A PROFESSIONAL ENGINEER

SEE DETAIL "A"

(I)·

PRECAST REINFORCED CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED **CONCRETE BASE 2**

2" (TYP)

" OVERHANGING

- PRECAST REINFORCED

CONCRETE FLAT SLAB TOP

1/2" CEMENT

- MORTAR

BEVEL 45°

2 COURSES 으는

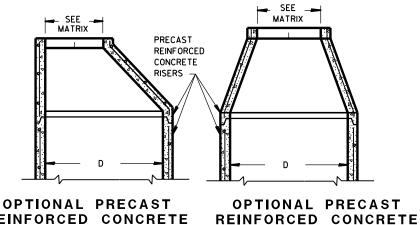
12'. EPT

6" BLOCK

4" MIN

SPLIT PIPE OR FORM CONCRETE TO FIT

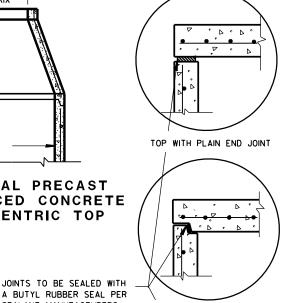
PLASTER COAT



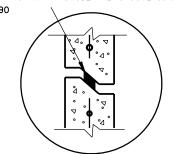
REINFORCED CONCRETE **ECCENTRIC TOP** CONCENTRIC TOP

PRECAST

WALL

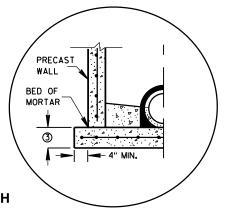


A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS TOP WITH TONGUE AND GROOVE JOINT RECOMMENDATIONS CONFORMING TO ASTM C990



RISER WITH TONGUE AND GROOVE JOINT

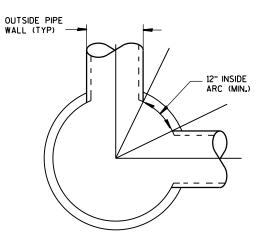
DETAIL "B"



PRECAST REINFORCED

CONCRETE WITH INTEGRAL BASE OPTION

SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION DETAIL "A"



DETAIL "C"

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L". "CATCH BASINS 4-B". "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES. THE CONE TOPS SHALL BE INSTALLED ON A BED OF MORTAR.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING: PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2" AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

CONCRETE BLOCK WILL NOT BE PERMITED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT. 5 INCHES FOR 4-FT. 6 INCHES FOR 5-FT. 7 INCHES FOR 6-FT, 8 INCHES FOR 7-FT AND 9 INCHES FOR 8-FT DIAMETER PRECAST MANHOLES.
- (2) FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- (3) PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS

MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE	С	ALL J'S	К	L	M
OPENING SIZE (FT)					
2 DIA.	х	х		х	
3 DIA.			×		Х

PIPE MATRIX

MANHOLE	MAXIMUM INSIDE PIF FOR TWO F	
SIZE	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	36
7-FT	48	36
8-FT	60	42

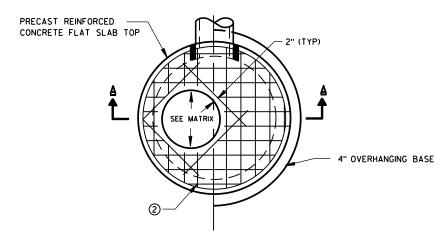
MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER

> STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED	
6/5/2012	/S/ Jerry H.Zogg
DATE	ROADWAY STANDARDS DEVELOPMENT
FHWA	ENGINEER

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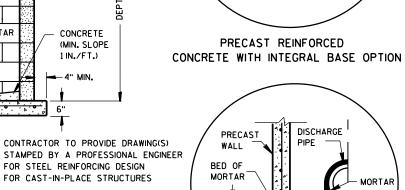
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PLAN VIEW CIRCULAR OPENING

JOINTS TO BE SEALED WITH

A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C990 (TYP) PRECAST DISCHARGE WALL TOP WITH PLAIN END JOINT



1/2" CEMENT

CONCRETE

(MIN. SLOPE 1 IN. /FT.)

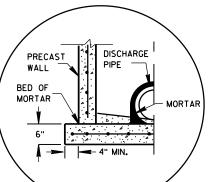
CONCRETE BLOCK

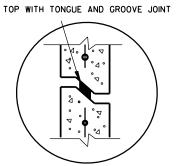
OR PRECAST REINFORCED

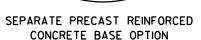
CONCRETE BASE 2

WITH CAST-IN-PLACE

PLASTER COAT







RISER WITH TONGUE AND GROOVE JOINT

DETAIL "A"

DETAIL "B"

INLETS 3-FT AND 4-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

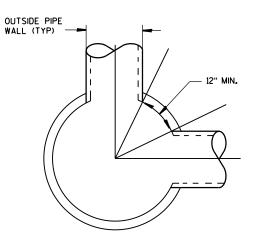
4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- (1) MINIMUM WALL THICKNESS SHALL BE 4-IN FOR 3-FT DIAMETER AND 5-IN FOR 4-FT DIAMETER PRECAST INLETS.
- (2) FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

INLET COVER OPENING MATRIX

	INLET COVER TYPE	ALL A'S	ALL B'S	BW	С	F	ALL H'S	S	T	٧	WM	Z
INLET SIZE	OPENING SIZE (FT)											
3-FT	2 DIA.				×							х
	2X2	х	х					х		Х		
4-FT	2 DIA.				×							Х
	2X2	х	x					х		х		
	2X2.5			Х				х	х	Х	Х	
	2X3						х					
	2.5X3					х						



DETAIL "C"

PIPE MATRIX

INLET	MAXIMUM INSIDE P FOR TWO	
SIZE	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18

INLETS 3-FT AND 4-FT DIAMETER

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

6/5/2012 /S/ Jerry H. Zogg DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER FHW4

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SEE DETAIL "A"

2

PRECAST REINFORCED

MONOLITHIC BASE

CONCRETE WITH

8 (1)

> DISCHARGE PIPE

SECTION A-A

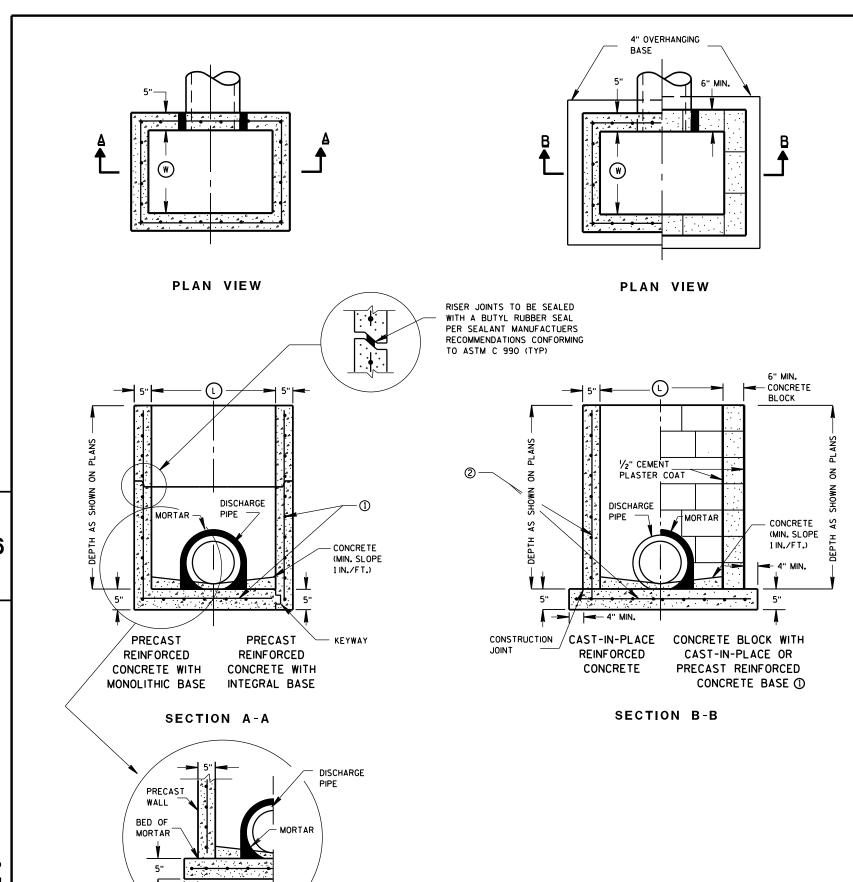
CIRCULAR INLETS W/ FLAT TOP

MORTAR

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DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

- 4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
- 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
- OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

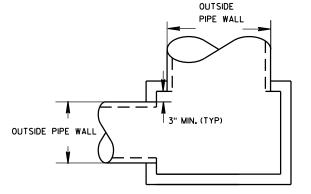
- 1) FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- (2) CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

	INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	s	т	v	WM
		WIDTH (W) (FT)	LENGTH (L) (FT)									
	2X2-FT	2	2	X	х				Х		х	
ſ	2X2.5-FT	2	2.5			Х			Х	Х	Х	Х
[2X3-FT	2	3					Х				
	2.5X3-FT	2.5	3				Х					

PIPE MATRIX

	MAXIMUM II Diam	NSIDE PIPE IETER
INLET SIZE	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED 6/5/2012 DATE

FHWA

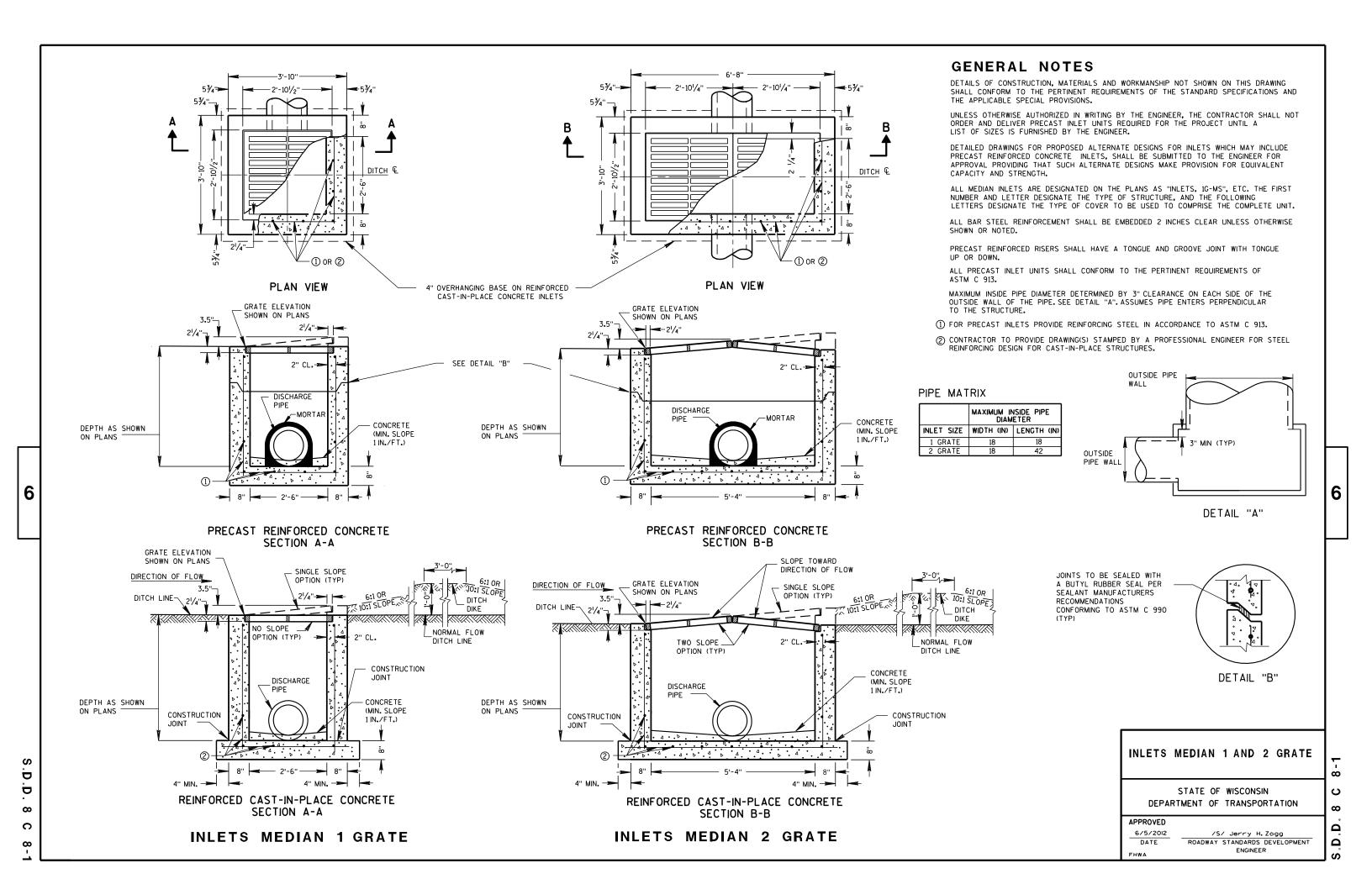
/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT

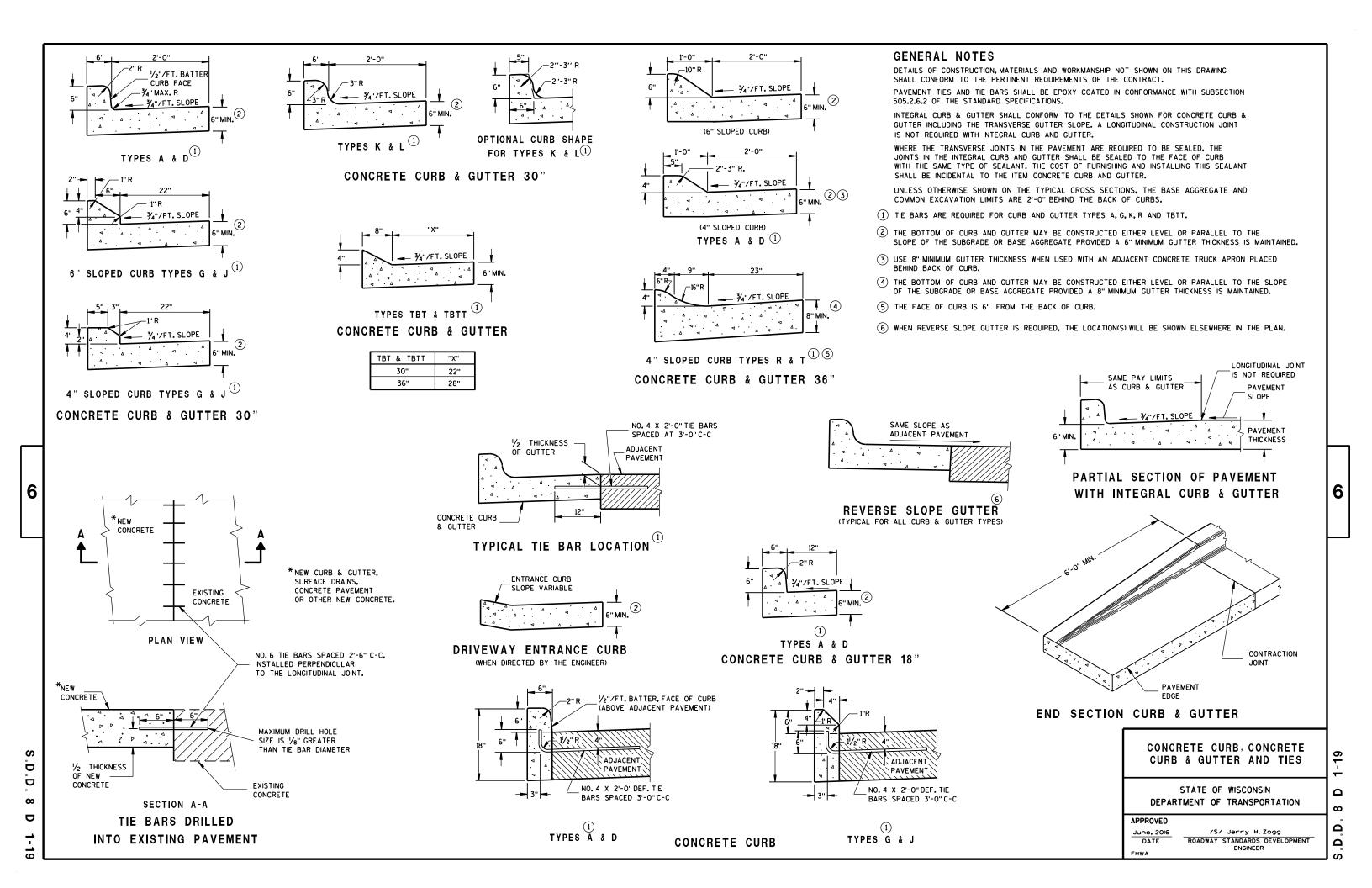
ENGINEER

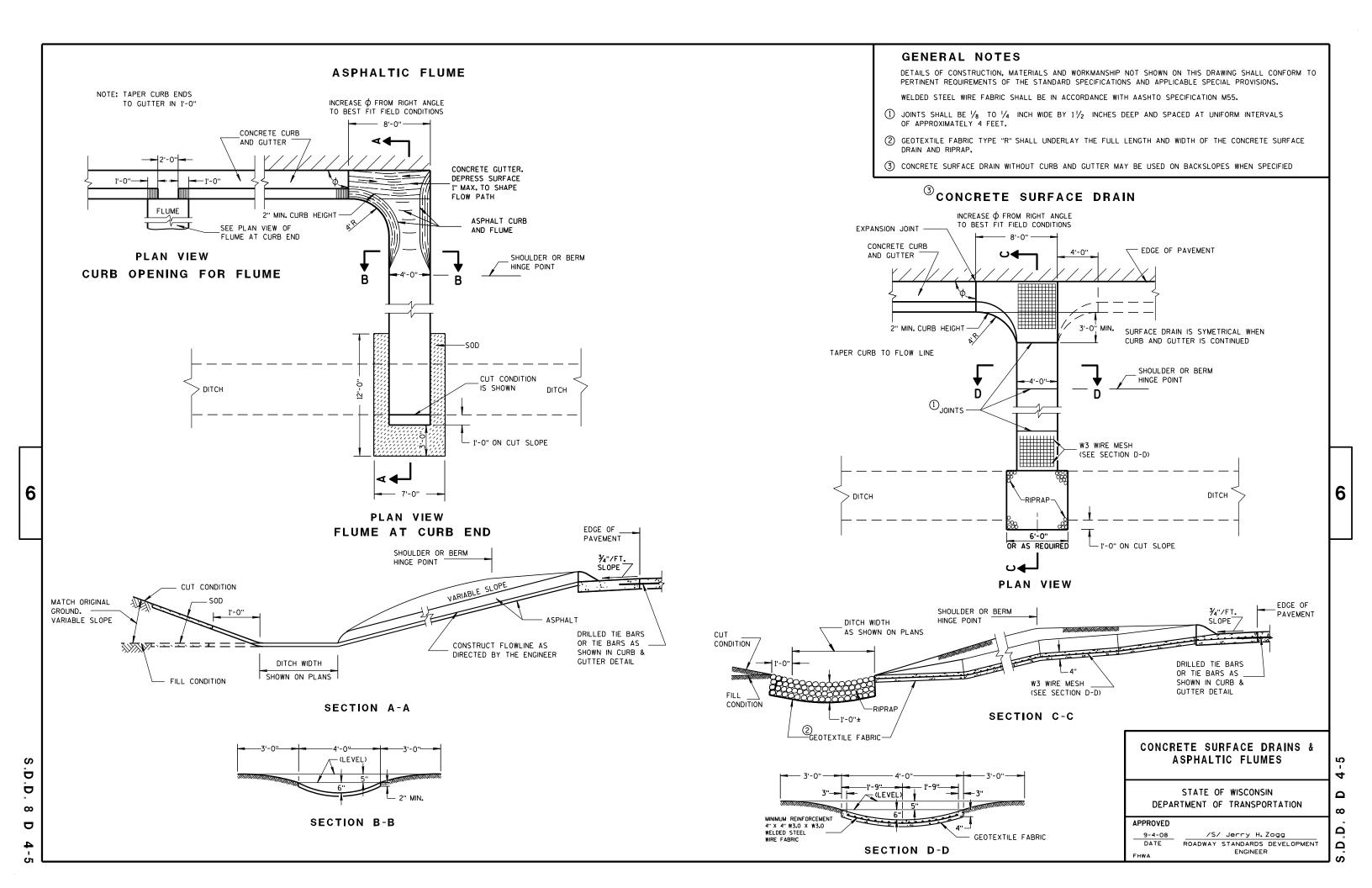
INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

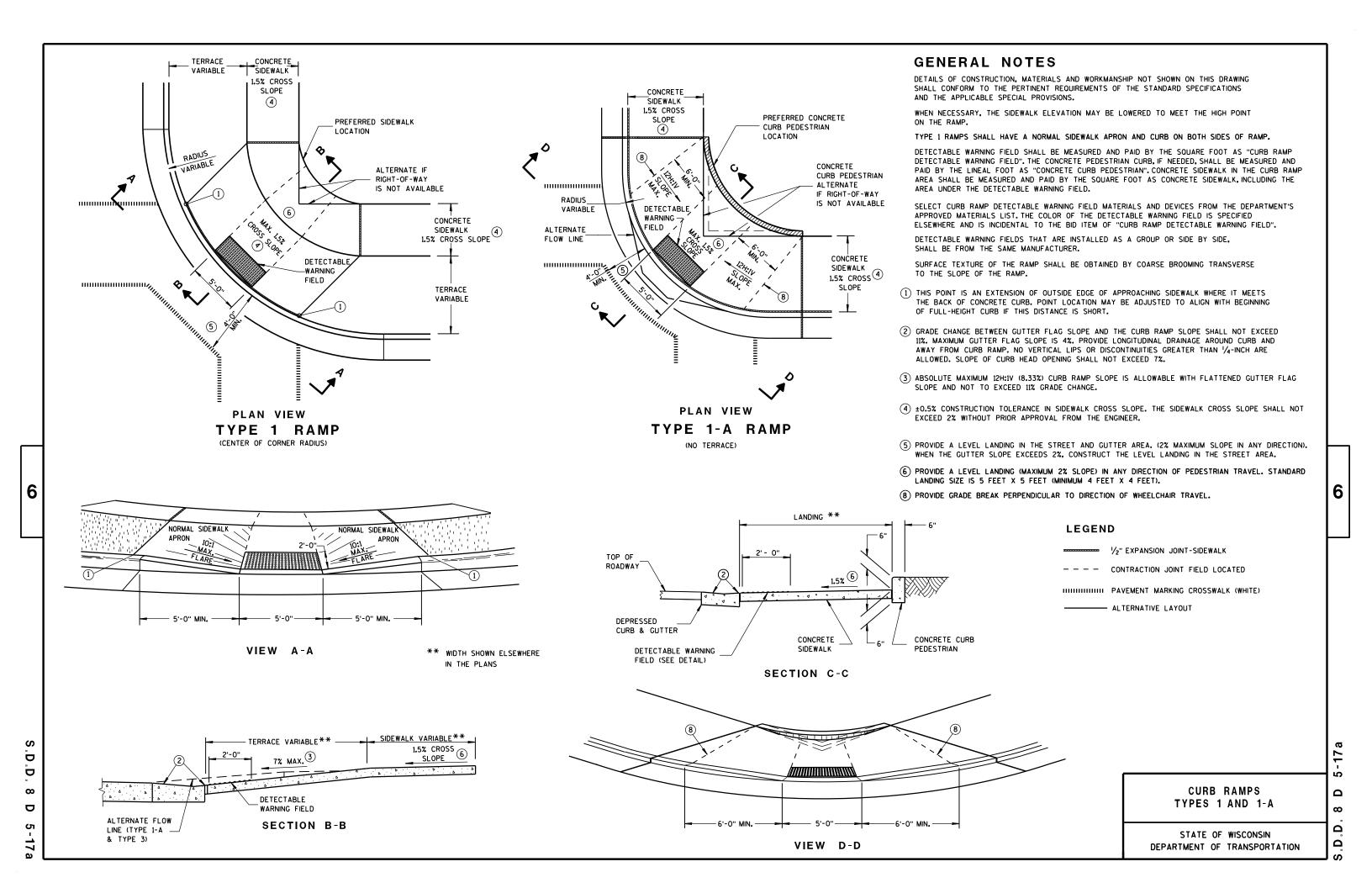
SEPARATE PRECAST REINFORCED

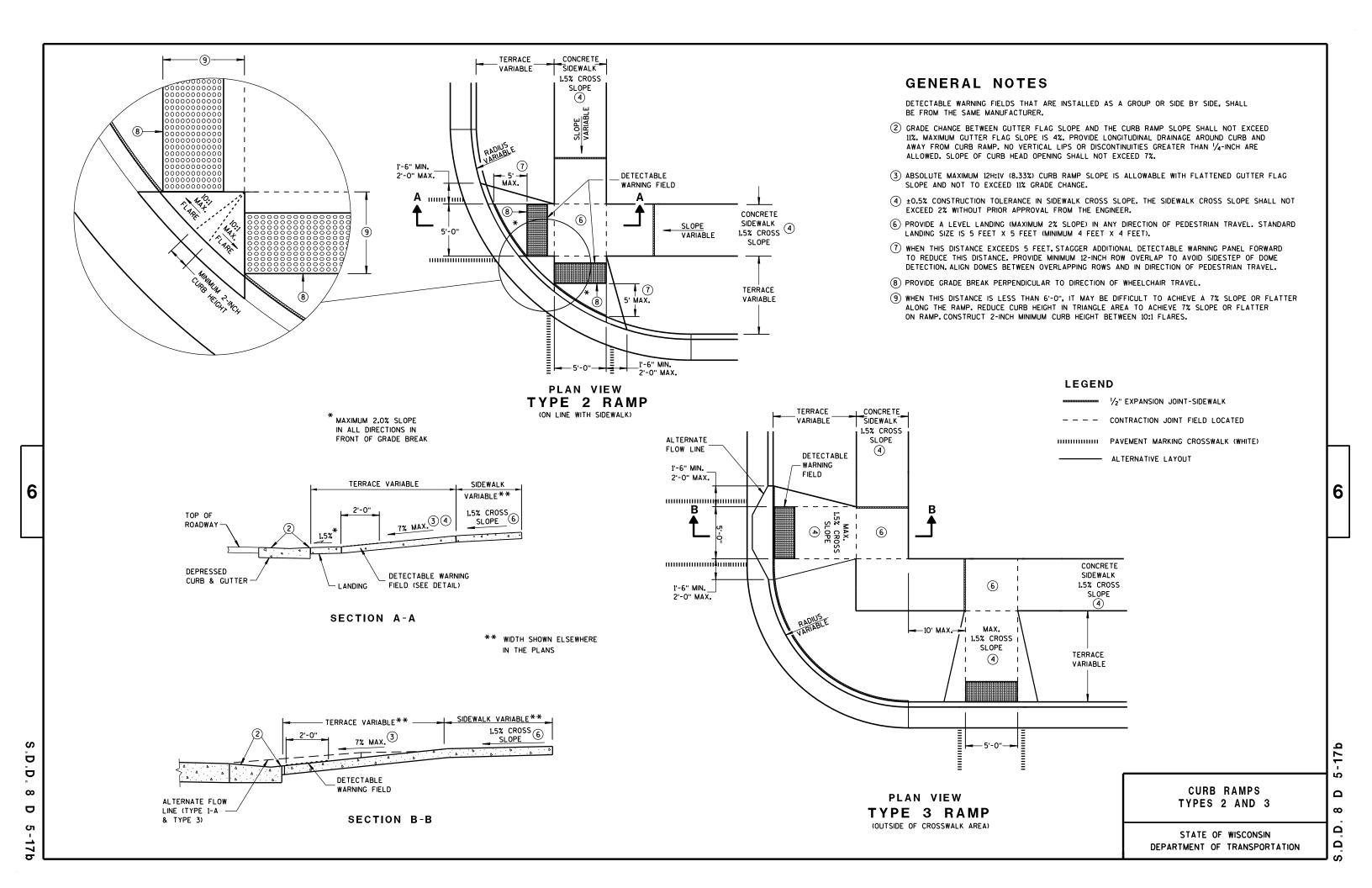
CONCRETE BASE OPTION

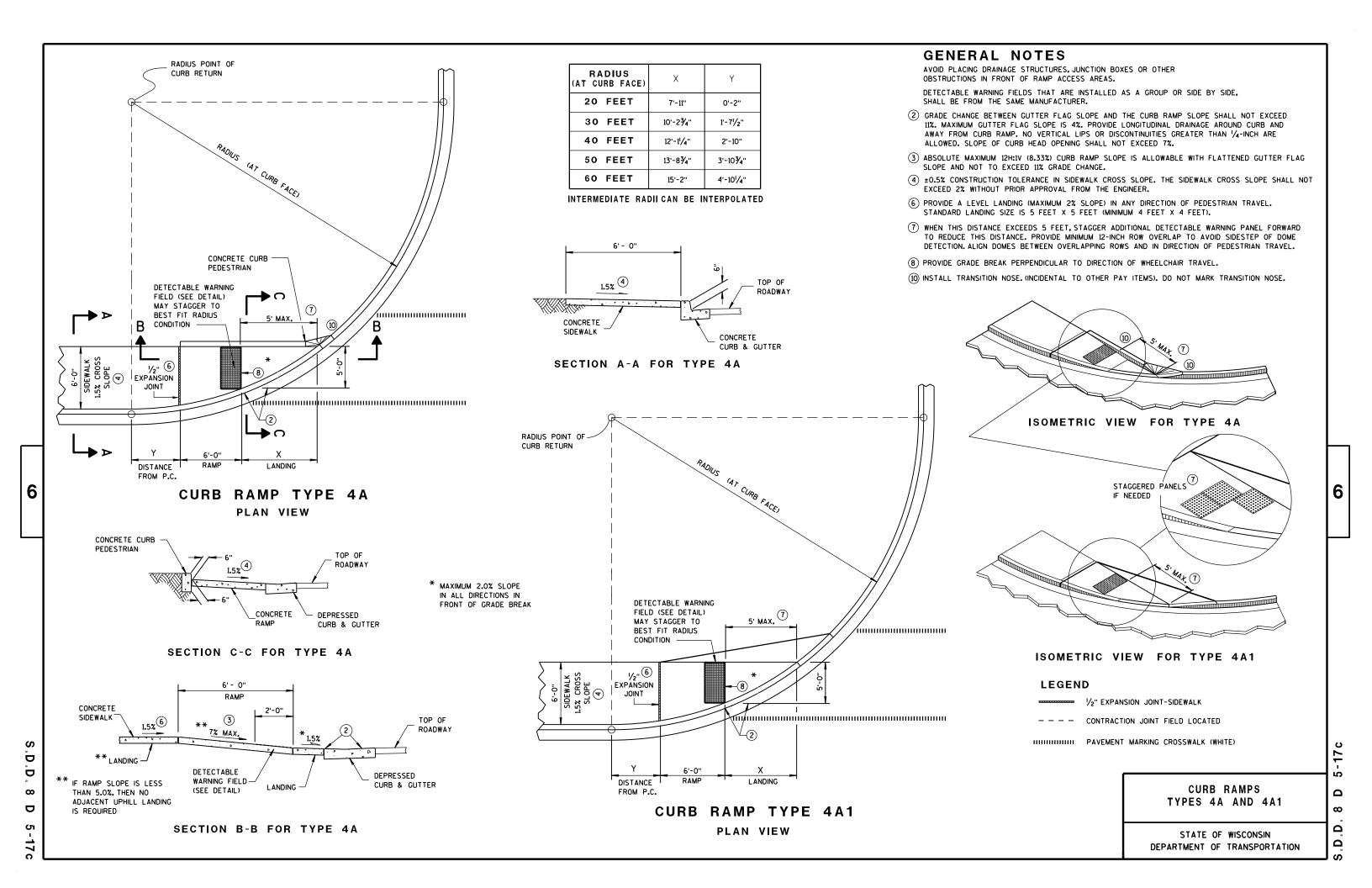


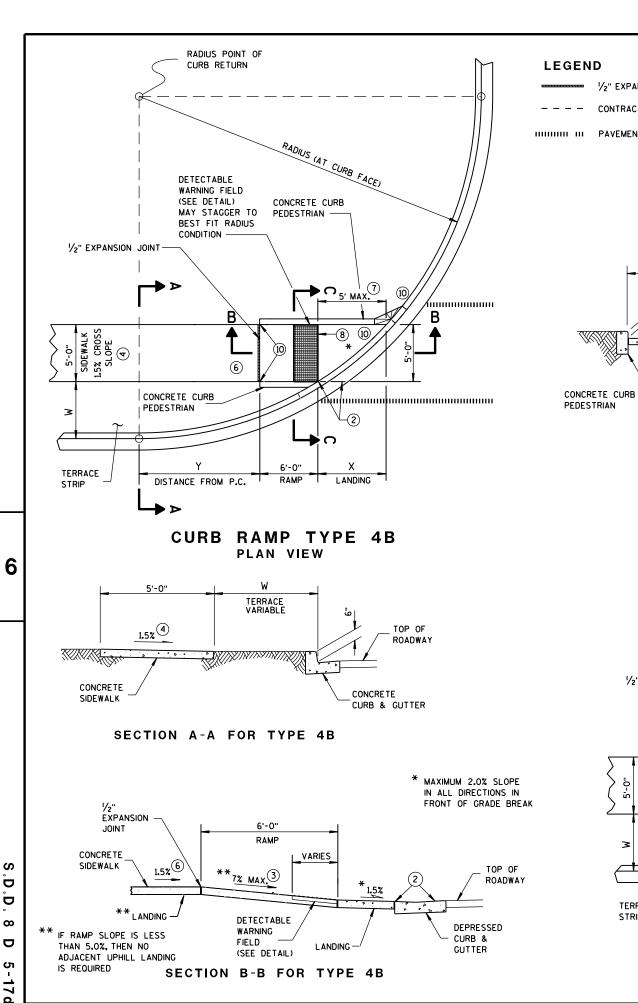












W = 5' - 0" 7' - Ø" 3' - Ø" W = 4' - Ø" W = 6' - 0"RADIUS AT CURB FACE ■ ½" EXPANSION JOINT-SIDEWALK 20 FEET 3'-8¾" 7'-6¾" 3'-61/2" 4'-111/2" 6'-51/2" 8'-61/4" 5'-9¾" 5'-13/4' 4'-31/4" 3'-3" CONTRACTION JOINT FIELD LOCATED 30 FEET 5'-101/2" 6'-91/2" 7'-11'/4" 6'-0'/4" 12'-5¾" 11'-13/4' HIHHHH HI PAVEMENT MARKING CROSSWALK (WHITE) 40 FEET 14'-1'/4" 15'-81/2" 50 FEET 9'-61/2" 9'-51/2" 12'-31/4" 8'-61/2" 14'-71/2" 7'-9¾" 16'-81/4" 7'-21/2" 18'-6'/4"

10'-61/2"

GENERAL NOTES

11'-0¾"

INTERMEDIATE RADII CAN BE INTERPOLATED

8'-1'/2"

21'-0'/2"

18'-11¾"

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

16'-81/2"

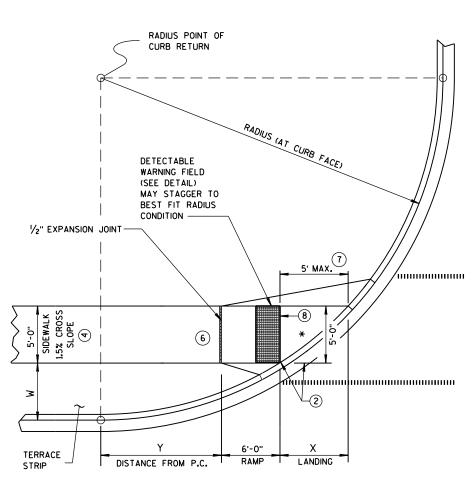
8'-9'/4"

9'-61/2"

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL NOT EXCEED 7%.
- 3 ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET (MINIMUM 4 FEET X 4 FEET).

14'-1'/4"

- (7) WHEN THIS DISTANCE EXCEEDS 5 FEET, STAGGER ADDITIONAL DETECTABLE WARNING PANEL FORWARD TO REDUCE THIS DISTANCE. PROVIDE MINIMUM 12-INCH ROW OVERLAP TO AVOID SIDESTEP OF DOME DETECTION. ALIGN DOMES BETWEEN OVERLAPPING ROWS AND IN DIRECTION OF PEDESTRIAN TRAVEL.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (10) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



60 FEET

TOP OF

ROADWAY

TERRACE STRIP

VARIES O TO W

CONCRETE
CURB & GUTTER

5'-0" RAMP

VARIES

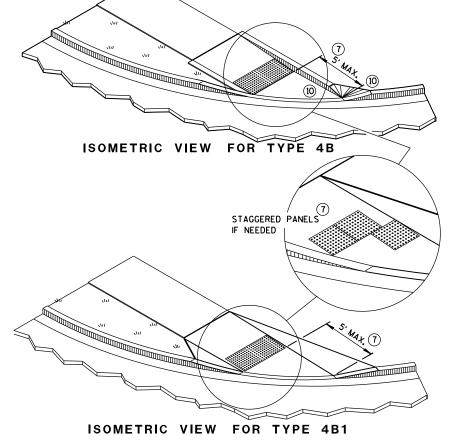
0 TO 6"

1.5%

SECTION C-C FOR TYPE 4B

11'-10'/4"

CURB RAMP TYPE 4B1
PLAN VIEW

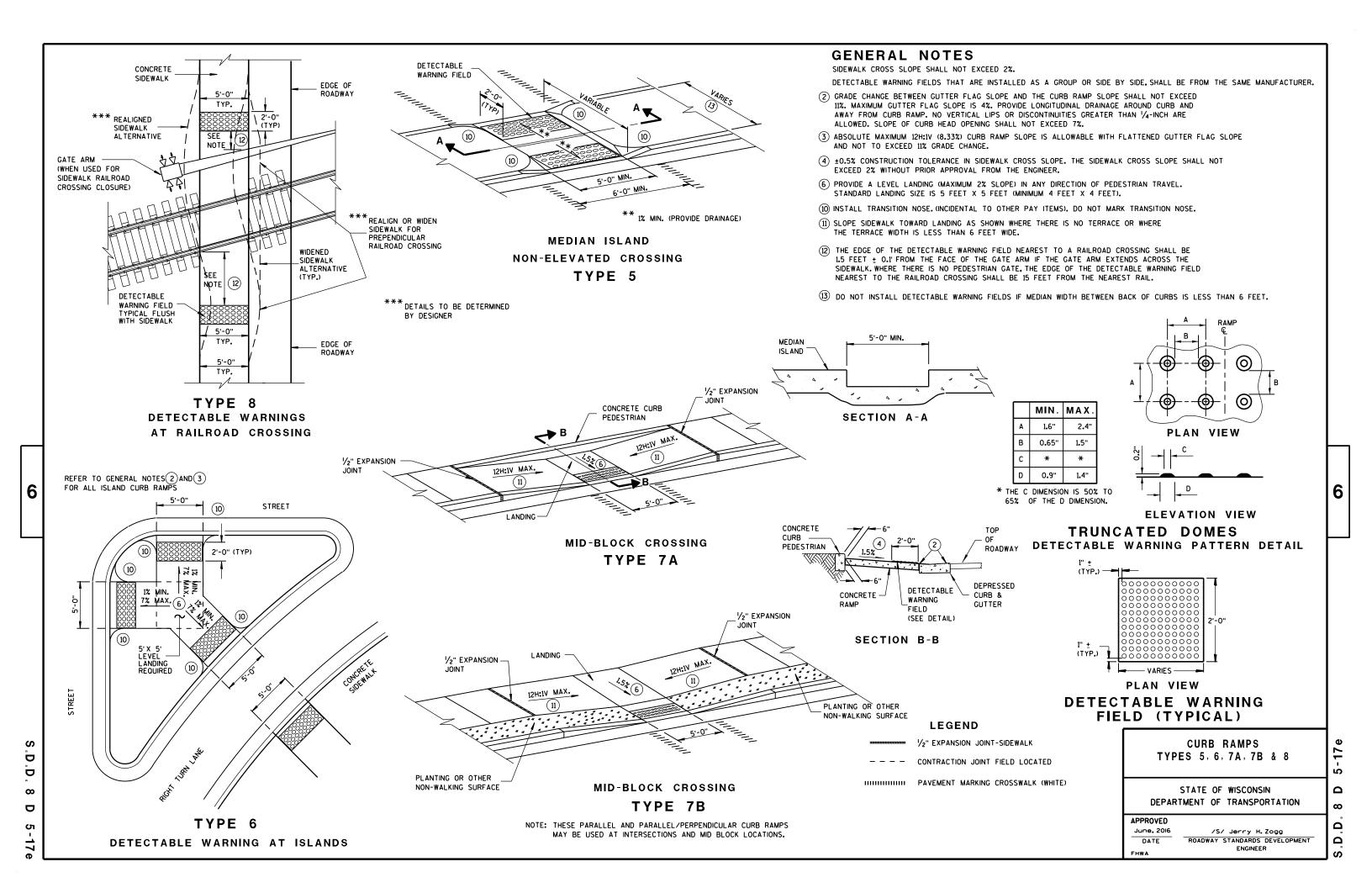


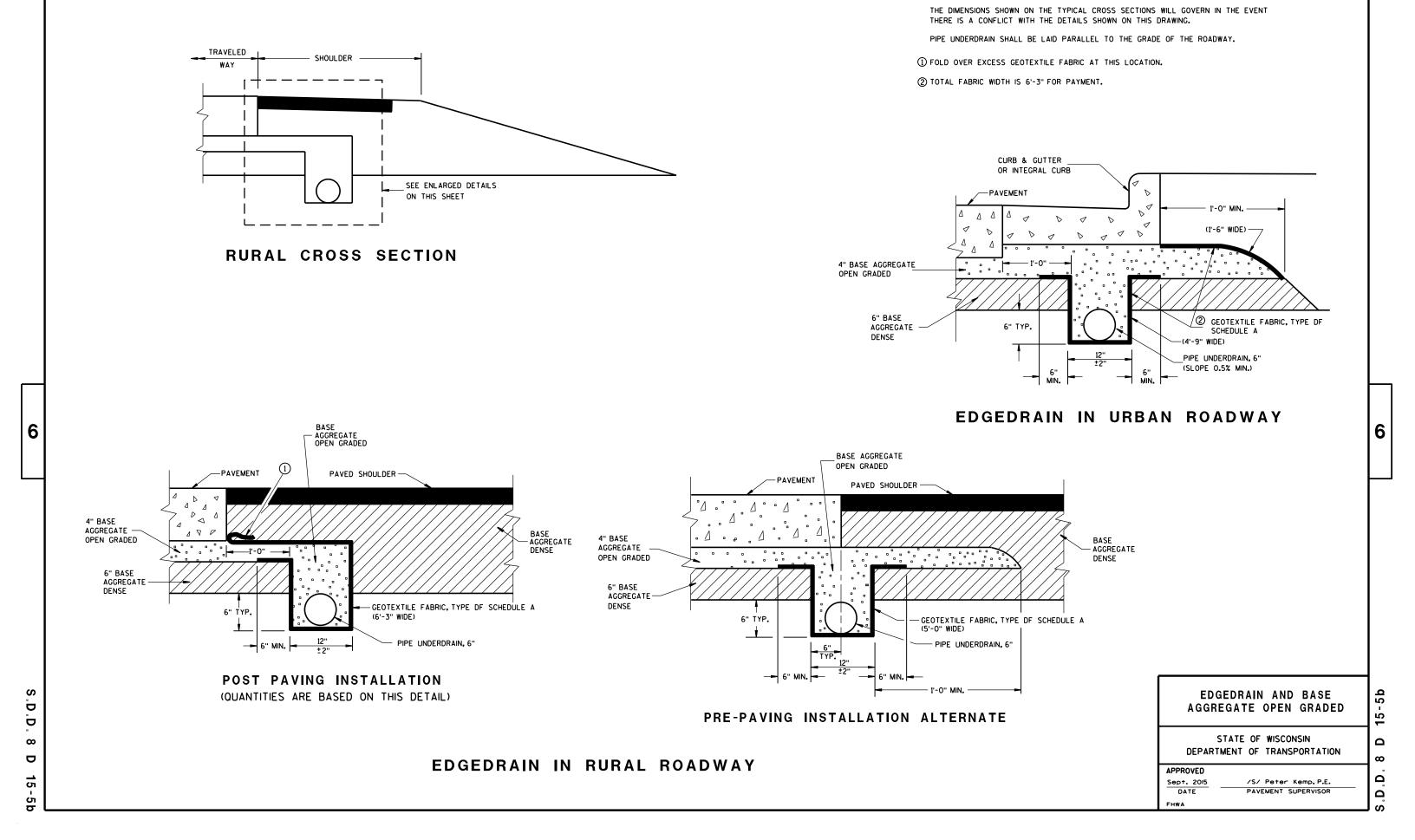
CURB RAMPS
TYPE 4B AND 4B1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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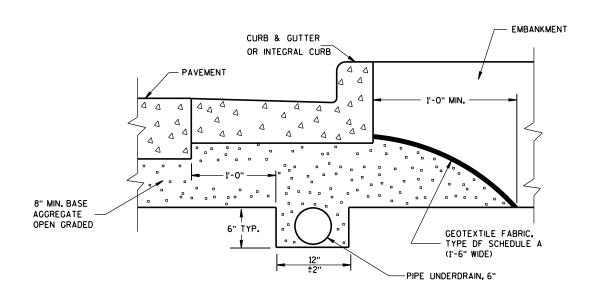


RURAL CROSS SECTION

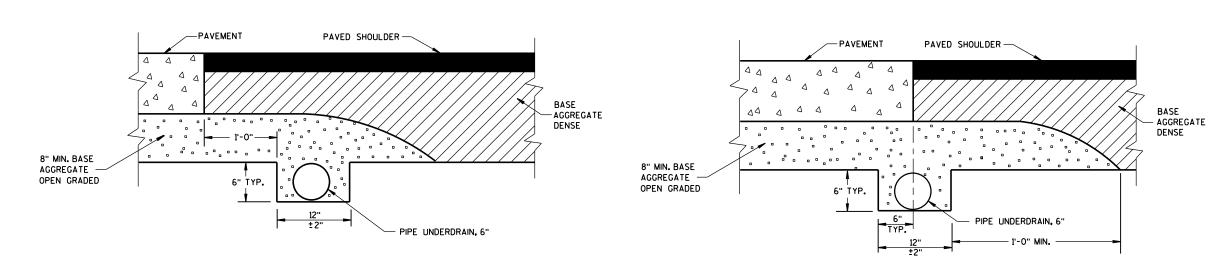
GENERAL NOTES

THE DIMENSIONS SHOWN ON THE TYPICAL CROSS SECTIONS WILL GOVERN IN THE EVENT THERE IS A CONFLICT WITH THE DETAILS SHOWN ON THIS DRAWING.

PIPE UNDERDRAIN SHALL BE LAID PARALLEL TO THE GRADE OF THE ROADWAY.



EDGEDRAIN IN URBAN ROADWAY



POST PAVING INSTALLATION (QUANTITIES ARE BASED ON THIS DETAIL)

PRE-PAVING INSTALLATION ALTERNATIVE

EDGEDRAIN IN RURAL ROADWAY

EDGEDRAIN AND BASE AGGREGATE OPEN GRADED

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED		
Sept. 2015	/S/ Peter Kemp, P.E.	
DATE	PAVEMENT SUPERVISOR	

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DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF **EROSION BALES / TEMPORARY** DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 /S/ Beth Connestro
CHIEF ROADWAY DEVELOPMENT ENGINEER

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TYPICAL APPLICATION OF SILT FENCE

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PLAN VIEW SILT FENCE AT MEDIAN SURFACE DRAINS



GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- \bigcirc HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- 3 WOOD POSTS SHALL BE A MINIMUM SIZE OF 11/8" X 11/8" OF OAK OR HICKORY.
- 4) SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- (5) CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Cannestra

29-05 /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER

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INLET PROTECTION, TYPE A

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- 1) FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- (2) FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- (3) FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE, THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION TYPE A, B, C, AND D

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02

/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER 6

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			1	METAL	APR	ON EN	NDWAL	.LS			
PIPE	MIN. 1	THICK.			DIMEN:	SIONS (I	nches)			APPROX.	
DIA.	(Incl		A	В	Н	L	Γį	L ₂	W	SLOPE	BODY
(IN.)	STEEL	ALUM.	(±1")	(MAX.)	(±1")	(±1 ½")	①	0	(±2")	320.2	
12	.064	.060	6	6	6	21	12	171/2	24	2½+o 1	1Pc.
15	.064	.060	7	8	6	26	14	213/4	30	21/2+o 1	1Pc.
18	.064	.060	8	10	6	31	15	281/4	36	21/2+o 1	1Pc.
21	.064	.060	9	12	6	36	18	295/8	42	21/2+o 1	1Pc.
24	.064	.075	10	13	6	41	18	371/4	48	21/2+o 1	1Pc.
30	.079	.075	12	16	8	51	18	521/4	60	21/2+0 1	1Pc.
36	.079	. 105	14	19	9	60	24	59¾	72	21/2+o 1	2 Pc.
42	.109	.105	16	22	11	69	24	75%	84	21/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 ¹ / ₄ +o 1	3 Pc.
54	.109	.105	18	30	12	84	30	851/2	102	2 ¹ / ₄ †o 1	3 Pc.
60	.109×	.105×	18	33	12	87	_	_	114	2 to 1	3 Pc.
66	.109×	.105×	18	36	12	87	_	_	120	2 to 1	3 Pc.
72	.109×	.105×	18	39	12	87	_	_	126	2 to 1	3 Pc.
78	.109×	.105×	18	42	12	87	_	_	132	11/2+0 1	3 Pc.
84	.109×	.105×	18	45	12	87	_	_	138	11/2 to 1	3 Pc.
90	.109×	.105×	18	37	12	87	_	_	144	11/2+0 1	3 Pc.
96	.109×	.105×	18	35	12	87	_	_	150	1/2+0 1	3 Pc.

	RE	NFORC	ED C	ONCRET	E APRO	N E	NDWAL	.LS
PIPE			DIM	ENSIONS	(Inches)			APPROX.
DIA.	T	A	В	С	D	Ε	G	SLOPE
12	2	4	24	48 1/8	721/8	24	2	3 to 1
15	21/4	6	27	46	73	30	21/4	3 to 1
18	21/2	9	27	46	73	36	21/2	3 to 1
21	23/4	9	36	371/2	731/2	42	23/4	3 to 1
24	3	91/2	431/2	30	731/2	48	3	3 to 1
27	31/4	101/2	491/2	24	731/2	54	31/4	3 to 1
30	$3\frac{1}{2}$	12	54	193/4	731/2	60	31/2	3 to 1
36	4	15	63	34¾	97¾	72	4	3 to 1
42	$4\frac{1}{2}$	21	63	35	98	78	41/2	3 to 1
48	5	24	72	26	98	84	5	3 to 1
54	51/2		65	**************************************	8 ¹ / ₄ - 100	90	51/2	2% to 1
60	6	* ** 30-35	60	39	99	96	5	2 to 1
66	61/2	* ** 24-30	* * * 72-78	* * * 21-27	99	102	51/2	2 to 1
72	7	* ** 24-36	78	21	99	108	6	2 to 1
78	71/2	* ** 24-36	78	21	99	114	61/2	2 to 1
84	8	36	901/2	21	1111/2	120	61/2	1½+o 1
90	81/2	41	871/2	24	1111/2	132	61/2	11/2+0 1

THREADED %6" DIA. ROD CONNECTOR AROUND CULVERT & THROUGH TANK TYPE CONNECTOR LUG LUG OR ALTERNATE CONNECTOR STRAP (SEE DETAIL) MEASURED LENGTH OF CULVERT TYPE 1 FOR 12" THRU 24" CORR. PIPE







NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL. AND CORRUGATED BAND FITS INSIDE ENDWALL.

CORRUGATED PIPE. FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5

DIMPLED BAND MAY BE USED WITH HELICALLY

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT ALTERNATE FOR TYPE 1 CONNECTION END SECTION CONNECTOR STRAP

* EXCEPT CENTER PANEL SEE GENERAL NOTES





SHOULDER

SLOPE



SIDE ELEVATION METAL ENDWALLS



**MAXIMUM





CONCRETE ENDWALLS

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA, GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE

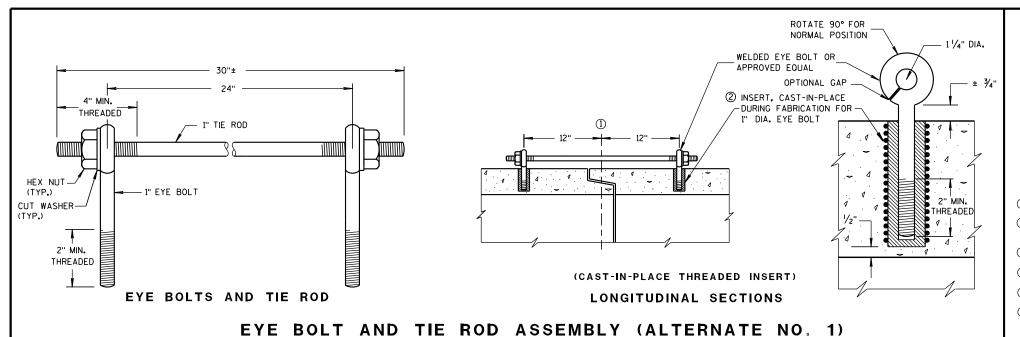
LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES. THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

(1) FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.



11/30/94 /S/ Rory L. Rhinesmith CHIEF ROADWAY DEVELOPMENT ENGINEER



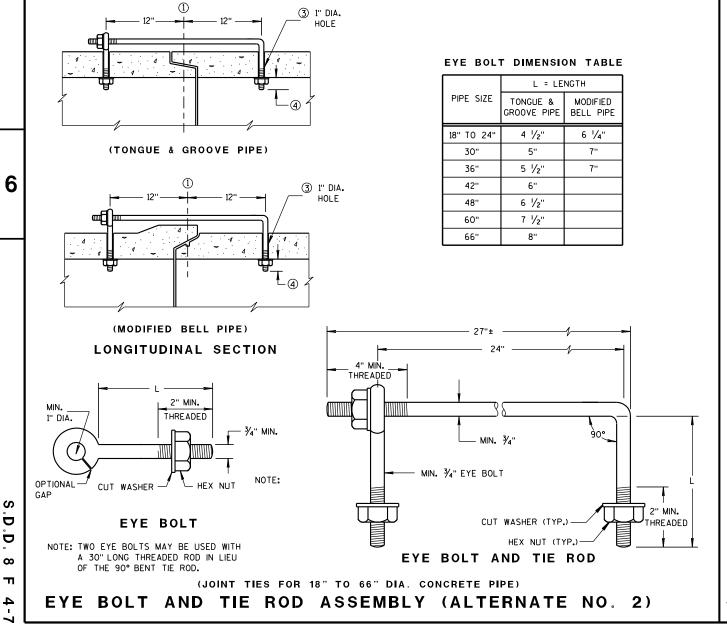
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES, ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

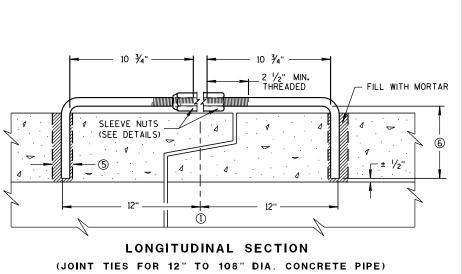
DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

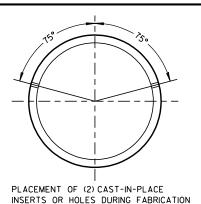
- (1) & OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE
- ${\mathfrak S}$ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ${\mathfrak L}$ OF TONGUE AND GROOVE.
- 4 BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- (5) OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN $rac{1}{2}$ INCH OF THE INNER SURFACE OF THE PIPE.



ADJUSTABLE TIE ROD TABLE 5/8 5 12-60 3/4 5 1/2 3/4 90-108 DIMENSIONS SHOWN ARE IN INCHES **TAPERED** PLAIN RIGHT AND LEFT THREADS **SLEEVE NUTS** 2 1/2" MIN. THREADED

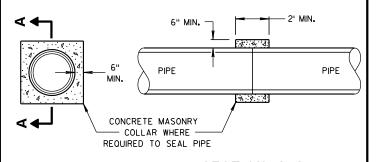


ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A-A

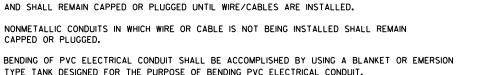
CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6/5/2012 /S/ Jerry H. Zogg DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

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TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L.LISTED ADAPTER FITTINGS SHALL BE USED.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES

SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REIN-STALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY

GENERAL NOTES

AND 36 INCHES MAXIMUM.

OF THE ENGINEER.

CAPPED OR PLUGGED.

MINIMUM AND 36 INCHES MAXIMUM.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

BOTTOM OF ¼" HOLE PVC CONDUIT-CONDUIT TRENCH FOR DRAINAGE NO. 2 COARSE AGGREGATE FILL —1'-0" DIA. OR SQUARE —>

NOTE: INSTALL AT LOCATIONS WHERE METALLIC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

BOTTOM OF

CONDUIT TRENCH

NOTE: INSTALL AT LOCATIONS WHERE PVC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

DRAIN SUMP FOR METALLIC CONDUIT

1'-0" DIA. OR SQUARE ──➤

METALLIC CONDUIT-

1" DIA. X 6"

NIPPLE

NO. 2 COARSE

AGGREGATE FILL

ARROW MARK SHALL BE INSCRIBED IN PAVEMENT SURFACE 1/4" TO 3/8"

DEEP AT EACH LOCATION WHERE CONDUITS ARE PLACED UNDER

PLAN VIEW

ARROW MARK

CONDUIT

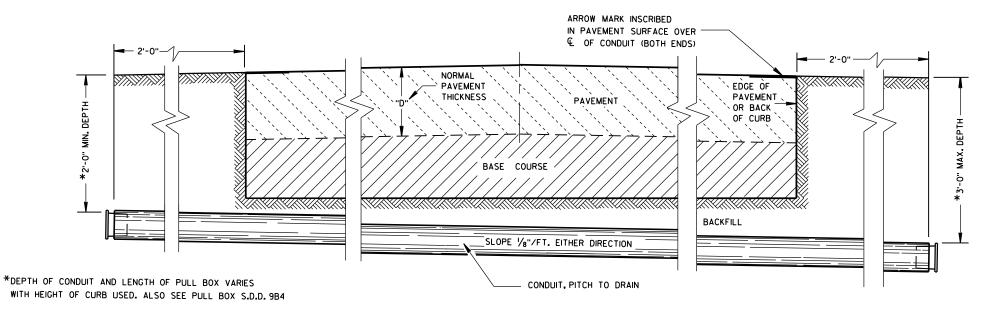
THE PAVEMENT

EDGE OF

PAVEMENT OR BACK

OF CURB

DRAIN SUMP FOR PVC CONDUIT



SIDE ELEVATION DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

CONDUIT

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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APPROVED /S/ Ahmet Demirbilek June. 2015 DATE STATE ELECTRICAL ENGINEER

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DIMENSION IN INCHES				COF	RRUGAT	ED ST	EEL P	IPE		
PIPE DIAMETER (INSIDE)	Α	12	12	12	18	18	18	24	24	24
PIPE LENGTH **	В	24	30	36	24	30	36	36	42	48
WALL THICKNESS	С	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 1/4	10 1/4	10 1/4	16 1/4	16 1/4	16 1/4	22 1/4	22 1/4	22 1/4
FRAME	Ε	14 1/2	14 1/2	14 1/2	20 ½	20 ½	20 ½	26 ½	26 ½	26 ½
FRAME	F	8 1/2	8 1/2	8 1/2	14 1/2	14 ½	14 1/2	20 ½	20 ½	20 ½
FRAME	G	11 1/2	11 1/2	11 1/2	17 1/2	17 1/2	17 1/2	23 ½	23 ½	23 ½
					WEIGH	T IN P	OUNDS	*		
FRAME AND COVER		60	60	60	110	110	110	155	155	155

- * THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.
- NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.

6" MAX. **EXTENSION** TOP OF ORIGINAL CORRUGATED PIPE (3) BOLTS, NUTS & LOCKWASHERS REQUIRED

ELECTRIC

FINAL GRADE

ALL METALLIC CONDUIT

AND THREADED

CUT OPENINGS

THE FIELD

2" PVC PIPE CAP ON BOTH ENDS

WITH 7, 8 1/4" HOLES DRILLED

IN EACH END.

PULL BOX

AS REQUIRED IN

ENDS SHALL BE REAMED

ALL CONDUIT PITCHED

4 TO 8 BRICKS

EQUALLY SPACED

TO DRAIN TO PULL BOXES

2" DRAIN DUCT TO

DITCH OR SEWER

WHEN SPECIFIED

CORRUGATED PIPE EXTENDER

HEAVY DUTY FRAME -

6" MIN.

(TYP.)

AND COVER

WHEN A PULL BOX IS INSTALLED IN CRUSHED

AGGREGATE SHOULDERS, PLACE IT 2-3

2-3 INCHES OF CRUSHED AGGREGATE

NO. 2 COARSE

(SEE SECTION 501

OF THE STANDARD

WIRE AND/OR CABLE.

INSTALL END BELLS (U.L. LISTED FOR

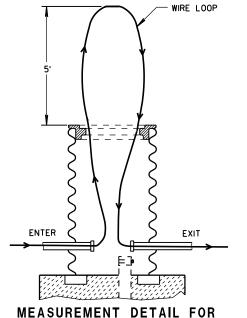
CONDUIT BEFORE INSTALLATION OF

ELECTRICAL USE) ON ALL NONMETALLIC

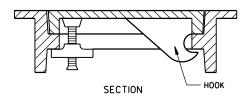
SPECIFICATIONS)

AGGREGATE

INCHES BELOW GRADE AND COVER IT WITH

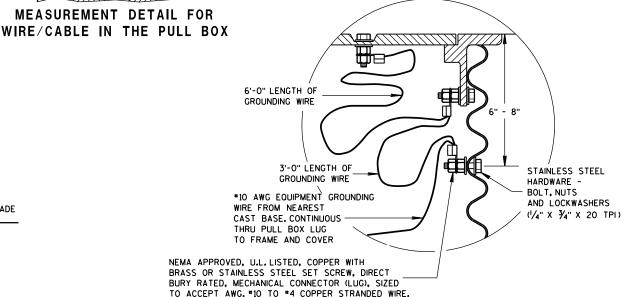


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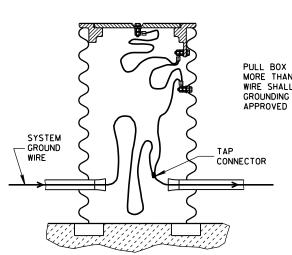


ALTERNATE COVER (LOCKING)

TIGHTENING BAR TYPE



EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES



EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES

PULL BOX TO NEAREST BASE DISTANCE MORE THAN 20 FEET. PULL BOX GROUND WIRE SHALL CONNECT AT SYSTEM GROUNDING WIRE. USE DEPARTMENT APPROVED TAP CONNECTOR.

PULL BOX

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

Sept. 2014 /S/ Ahmet Demirbilek DATE STATE ELECTRICAL ENGINEER FHWA

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED. SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.

TRAFFIC LOADS.

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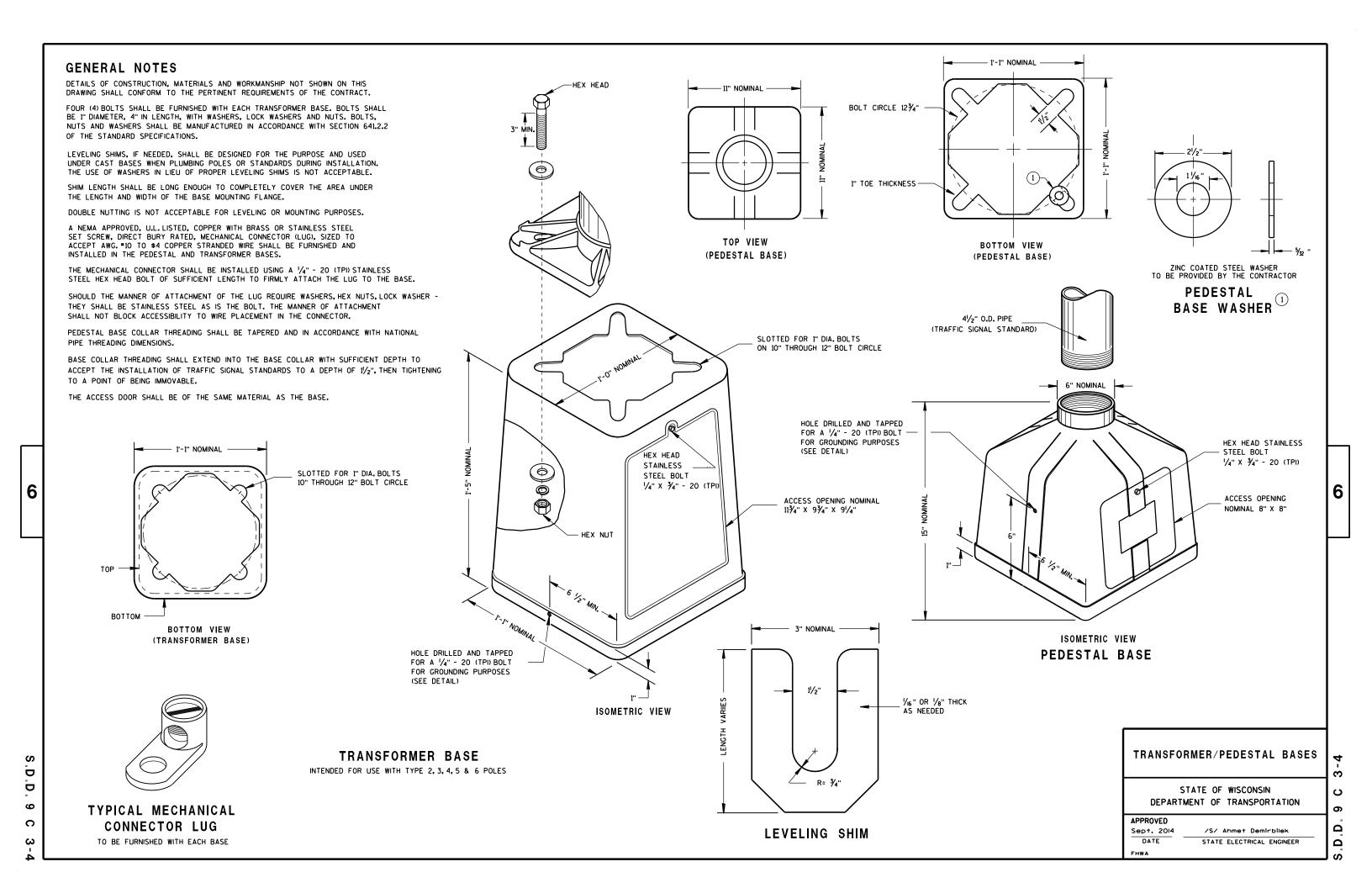
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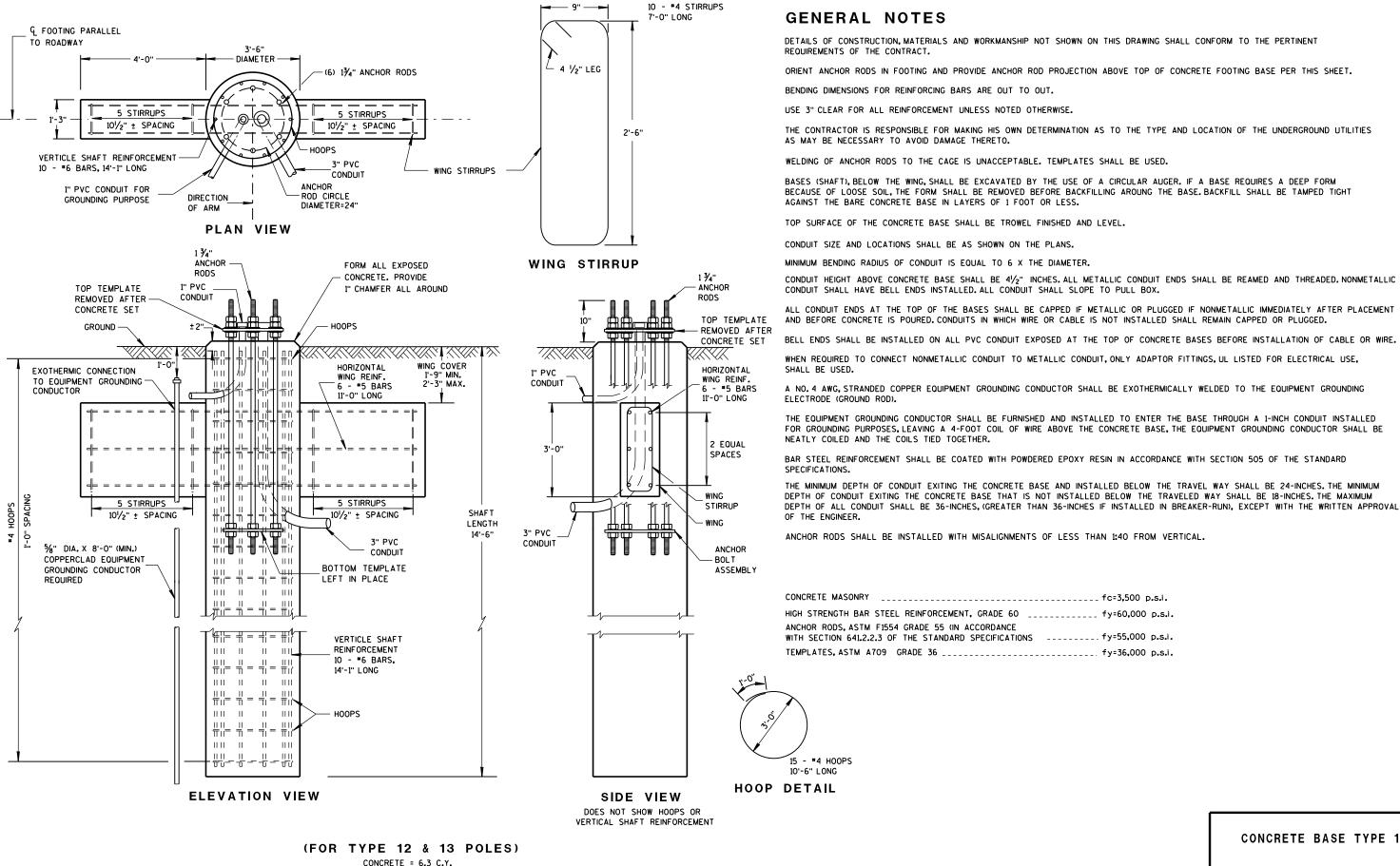
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H.S. REINFORCEMENT = 433 LBS.

SEE S.D.D. 9C13-2 WHEN GROUND ELEVATION AT BASE IS LOWER THAN HIGH POINT OF ROADWAY ELEVATION.

TO BE USED WHEN GROUND ELEVATION AT BASE EQUALS OR IS GREATER THAN HIGH POINT OF ROADWAY ELEVATION.

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CONCRETE BASE TYPE 13

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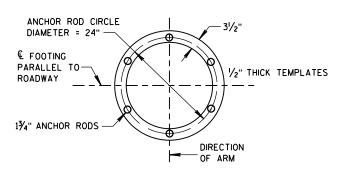
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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

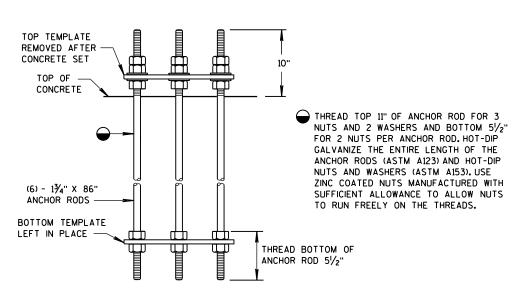






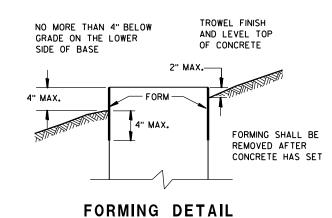


TOP AND BOTTOM TEMPLATES



ANCHOR BOLT ASSEMBLY DETAIL

CONCRETE BASE TYPE 13 ANCHOR ASSEMBLY



CONCRETE BASE TYPE 13

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION 6

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APPROVED May 2016 /S/ Ahmet Demirbilek DATE STATE ELECTRICAL ENGINEER FHWA

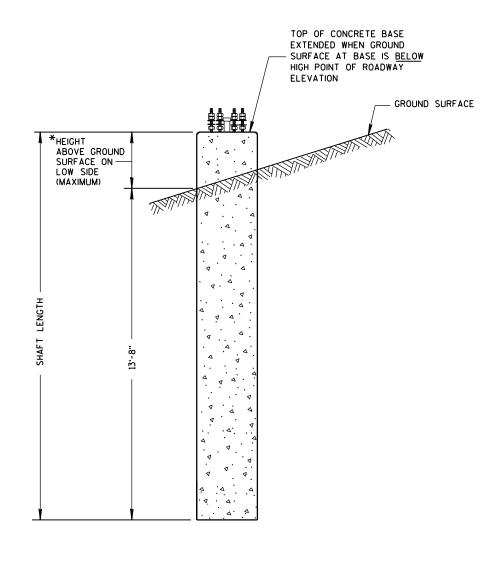
REINFORCEMENT AND CONCRETE QUANTITIES ADJUSTED FOR EXTENDED TYPE 10 CONCRETE BASE

HEIGHT INCREASE REQUIRED	* HEIGHT ABOVE GROUND SURFACE ON LOW SIDE (MAXIMUM)	SHAFT LENGTH	LENGTH OF *6 VERTICAL REINF.	NO. OF #4 HOOPS	C.Y. OF CONCRETE	LBS.OF HOOP BAR STEEL	LBS. OF VERTICAL BAR STEEL
>0" TO 6"	10"	14'-6"	14'-1"	16	2.6	78	127
>6" TO 1'-0"	1'-4"	15'-0"	14'-7"	16	2.7	78	131
>1'-0" TO 1'-6"	1'-10"	15'-6"	15'-1"	17	2.8	83	136
>1'-6" TO 2'-0"	2'-4"	16'-0"	15'-7"	17	2.9	83	141

REINFORCEMENT AND CONCRETE QUANTITIES ADJUSTED FOR EXTENDED TYPE 13 CONCRETE BASE

HEIGHT INCREASE REQUIRED	* HEIGHT ABOVE GROUND SURFACE ON LOW SIDE (MAXIMUM)	SHAFT LENGTH	LENGTH OF *6 VERTICAL REINF.	NO.OF #4 HOOPS	C.Y. OF CONCRETE	LBS. OF H.S. BAR STEEL
>0" TO 6"	10"	15'-0"	14'-7"	16	6.5	447
>6" TO 1'-0"	1'-4"	15'-6"	15'-1"	16	6.6	454
>1'-0" TO 1'-6"	1'-10"	16'-0"	15'-7"	17	6.8	469
>1'-6" TO 2'-0"	2'-4"	16'-6"	16'-1"	17	7.0	476

TOP OF CONCRETE BASE EXTENDED WHEN GROUND SURFACE AT BASE IS BELOW



CONCRETE BASE TYPE 10 (EXTENDED)

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HIGH POINT OF ROADWAY ELEVATION GROUND SURFACE *HEIGHT ABOVE GROUND SURFACE ON-LOW SIDE (MAXIMUM) 1'-9" MIN. & & FOOTING TYPE 10 & TYPE 13 EXTENSION

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

CONCRETE BASE

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APPROVED 11-26-2013 /S/ Ahmet Demirbilek DATE STATE ELECTRICAL ENGINEER FHWA

CONCRETE BASE TYPE 13 (EXTENDED)

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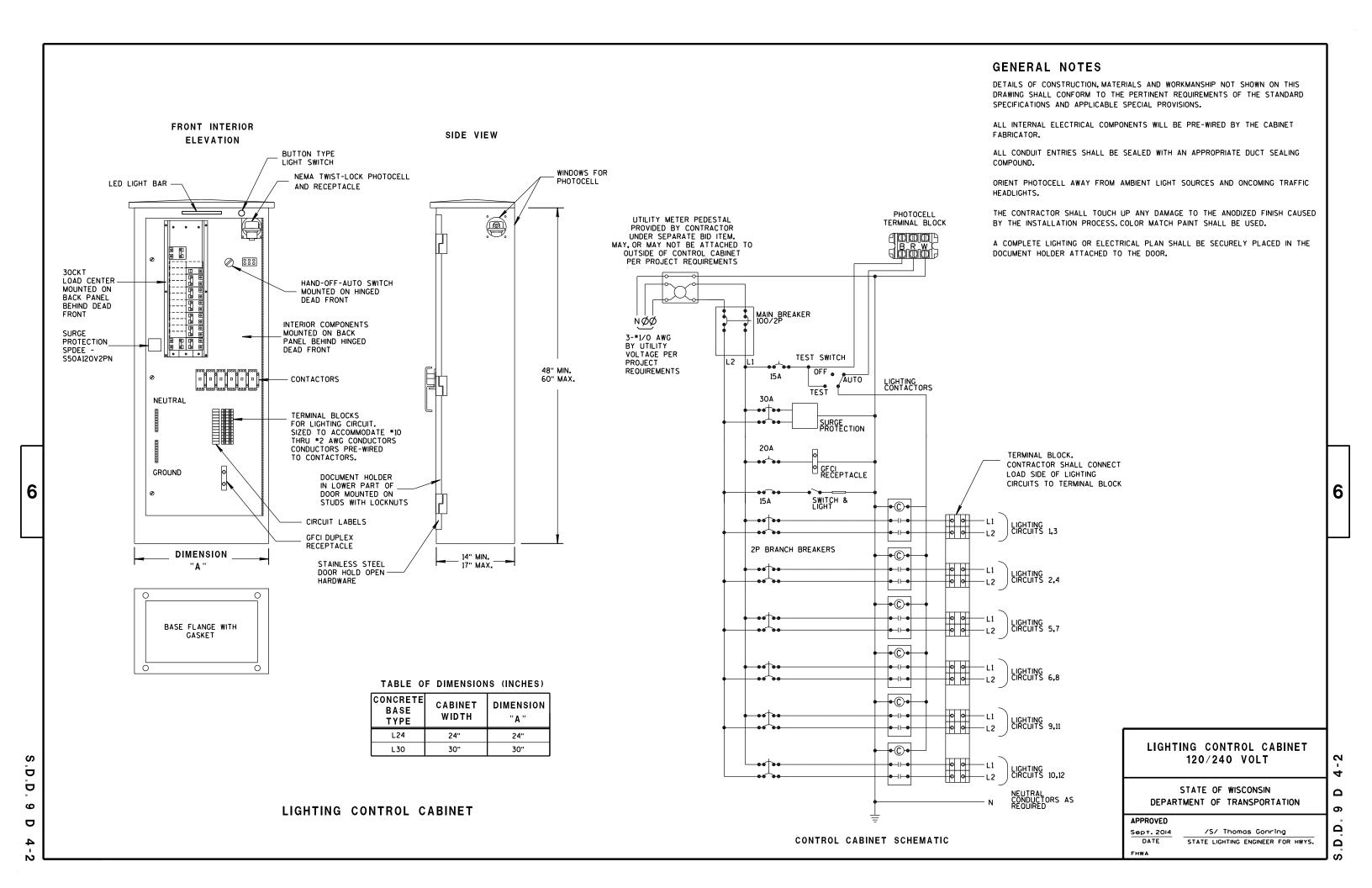
/S/ Ahmet Demirbilek

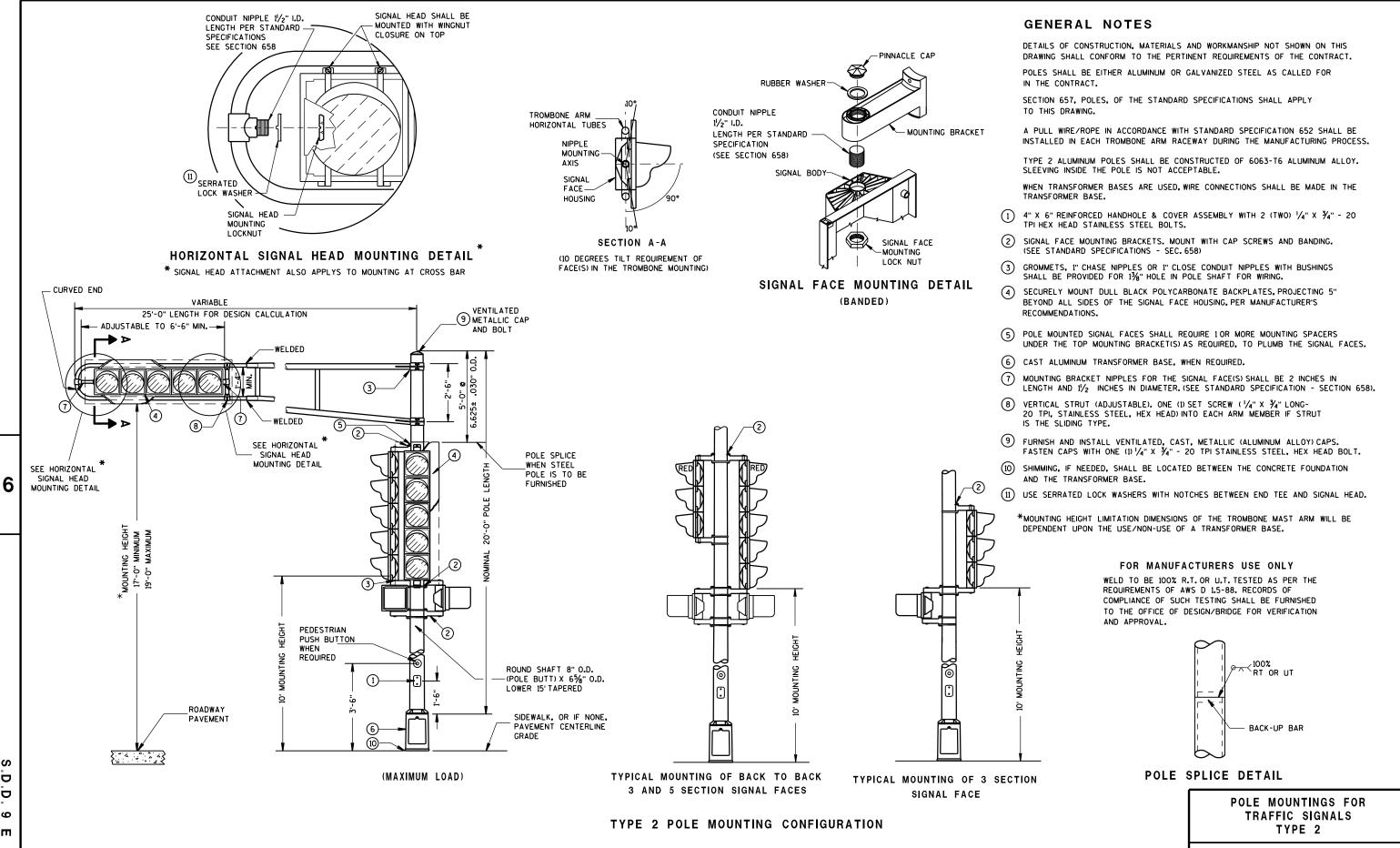
STATE ELECTRICAL ENGINEER

Sept. 2014

DATE

FHWA





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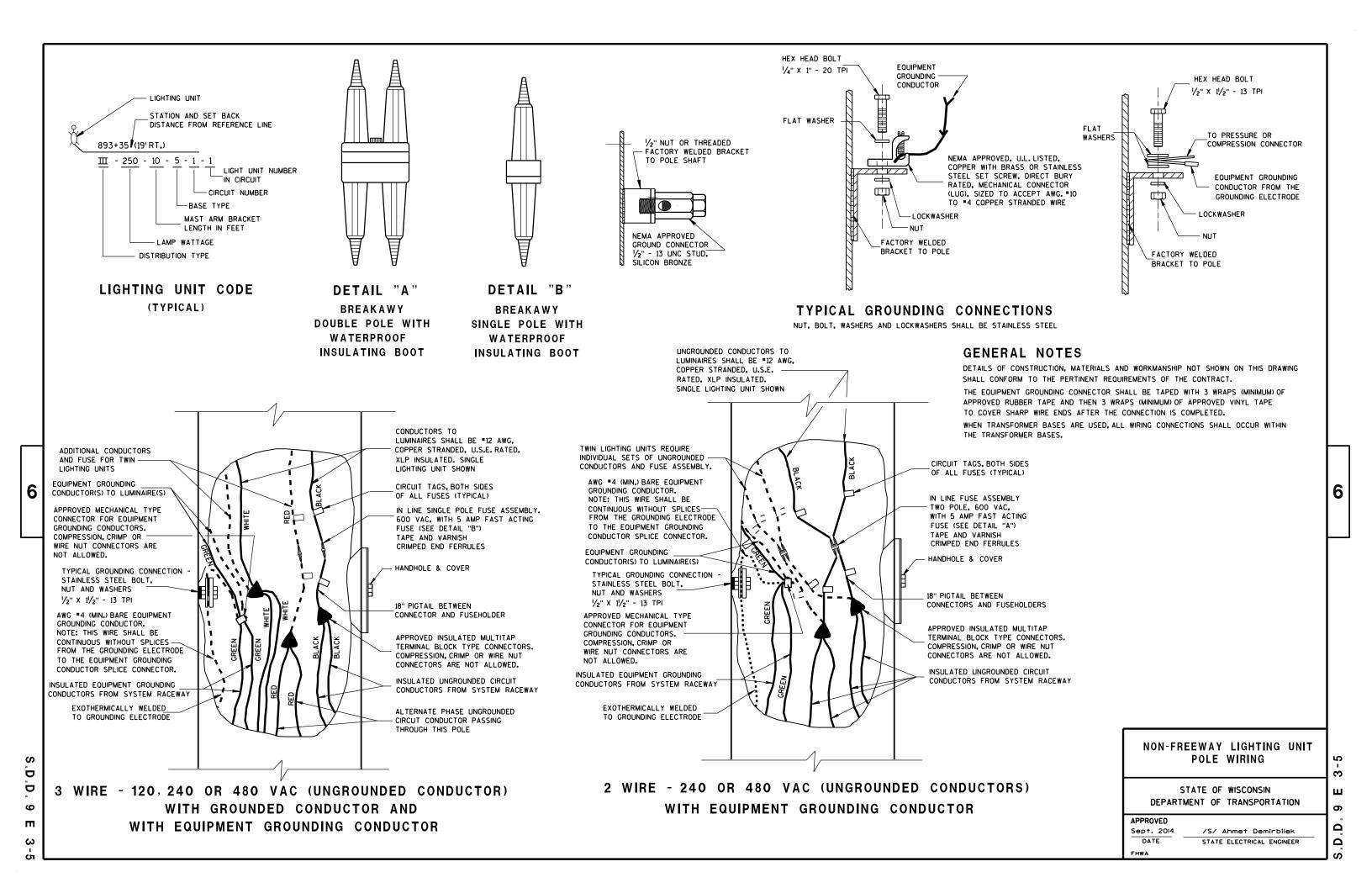
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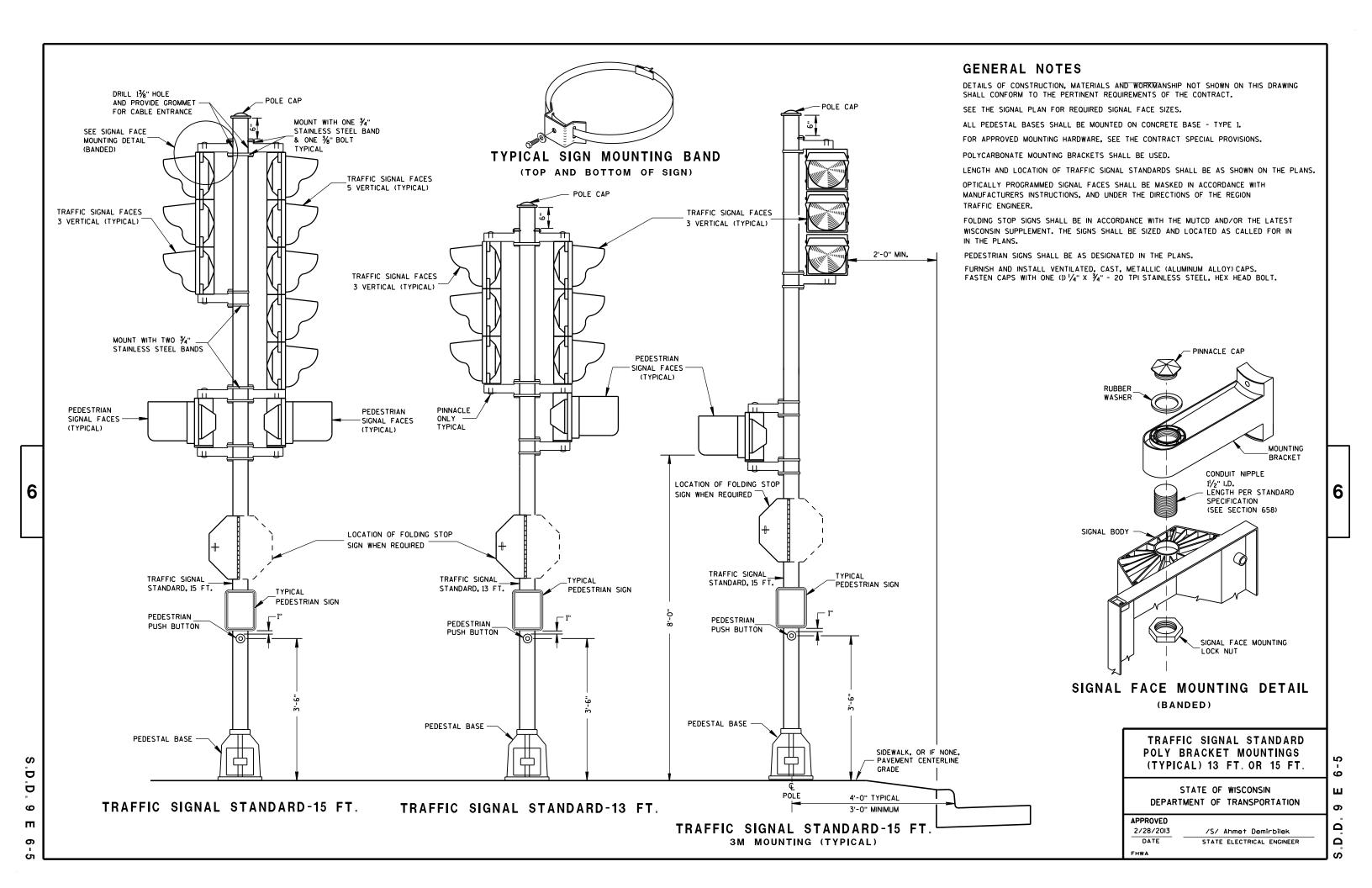
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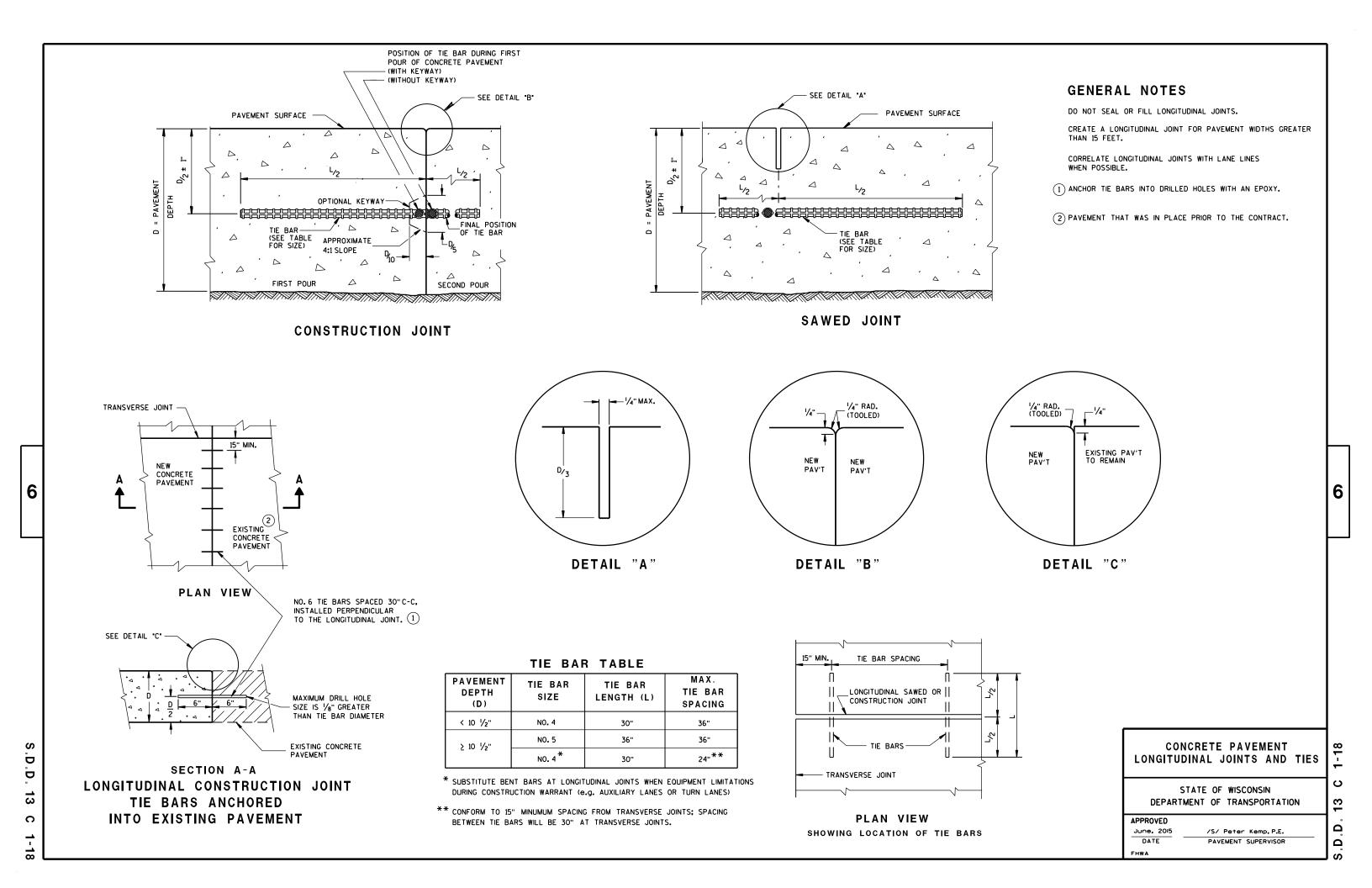
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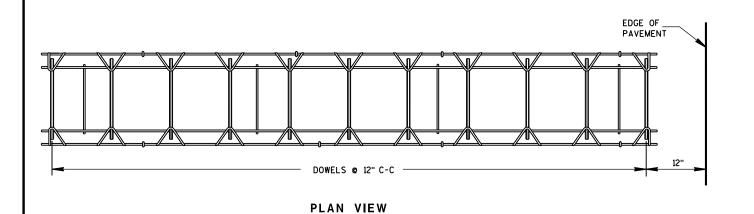
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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION









PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 1/2", 6",6 1/2"	NONE	12'
7",7 1/2"	1"	14'
8"•8 1/2"	1 1/4"	15'
9",9 1/2"	1 1/4"	15'
10" & ABOVE	1 1/2"	15'

GENERAL NOTES

CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT SEAL OR FILL CONTRACTION JOINTS.

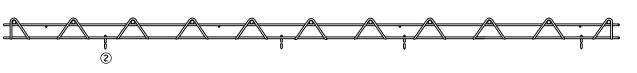
INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE LONGITUDINAL JOINT AND THE FREE EDGE

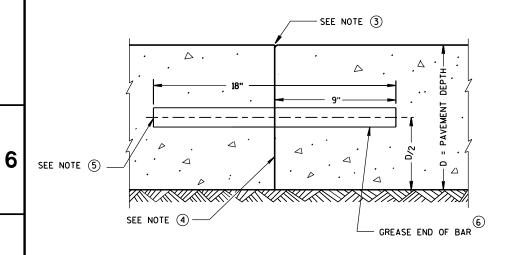
CONSTRUCTION JOINTS

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.

- (1) OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.
- 2) SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- (3) FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/4-INCH RADIUS AT FORMED JOINTS.
- 4 PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- 5 INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO DRILLED DOWEL BAR CONSTRUCTION JOINT DETAIL.
- 6 APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- (7) ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS 1/8-INCH GREATER THAN DOWEL BAR DIAMETER. 9 INCHES IN LENGTH.



SIDE VIEW CONTRACTION JOINT DOWEL ASSEMBLY



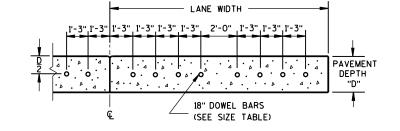
TRANSVERSE CONSTRUCTION JOINT

△ DOWEL BARS © 12" C-C 12" FROM PAVEMENT EDGE-

DOWELED CONTRACTION JOINT

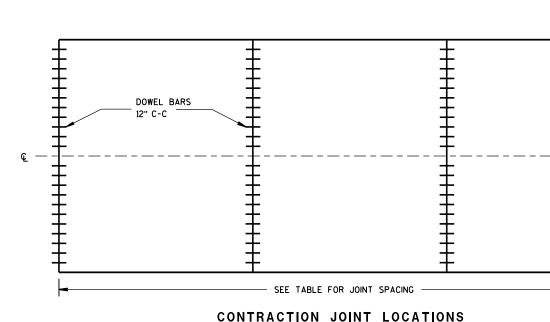
(SEE SIZE TABLE)

SEE JOINT DETAIL



(FOR 11' LANE WIDTH REDUCE CENTER SPACE TO 1'-O")

DRILLED DOWEL BAR CONSTRUCTION JOINT $^{\scriptsize \bigcirc}$



JOINT DETAIL

URBAN DOWELED CONCRETE PAVEMENT

- ¼" MAX.

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED 5/3/2013

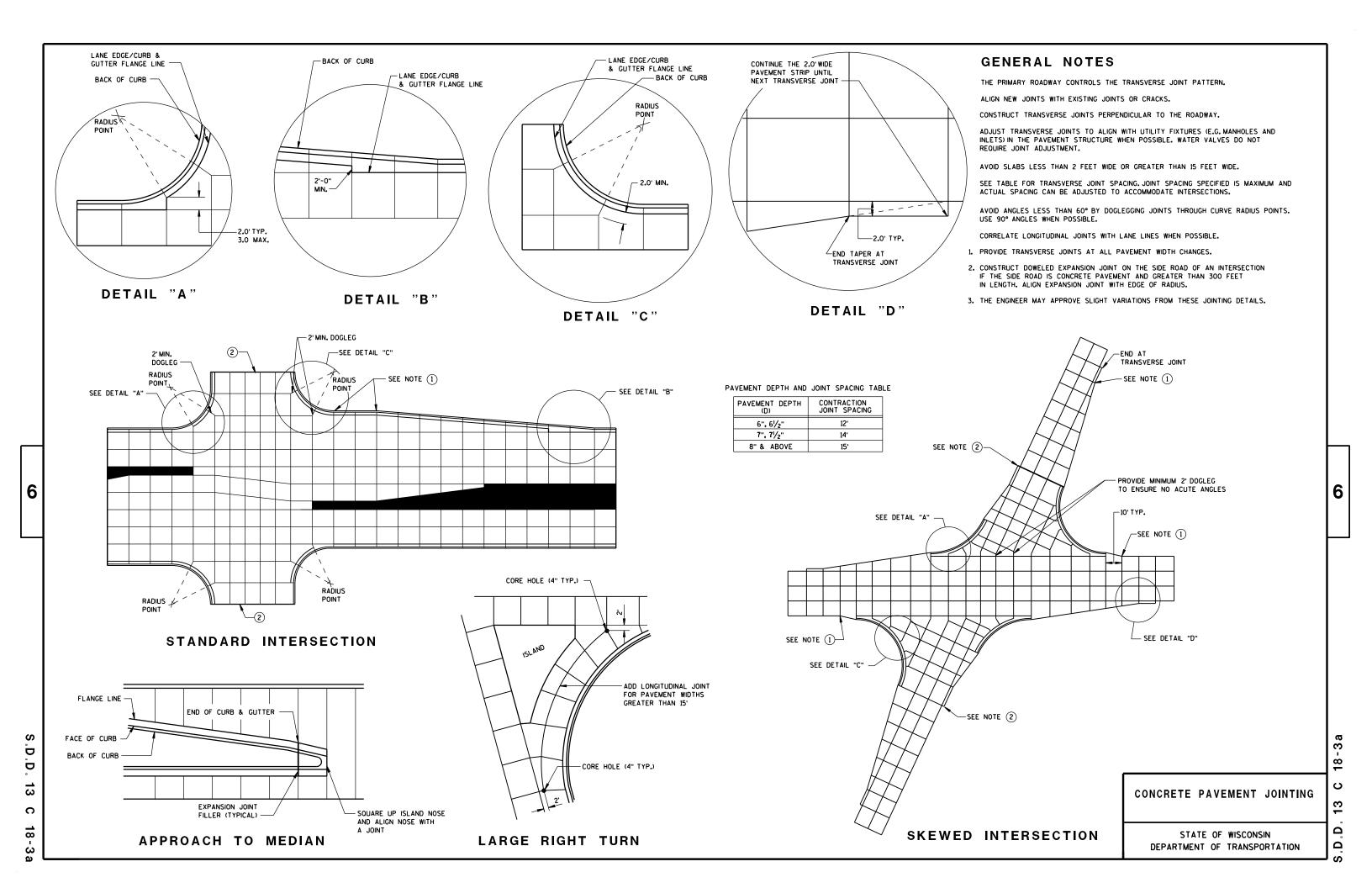
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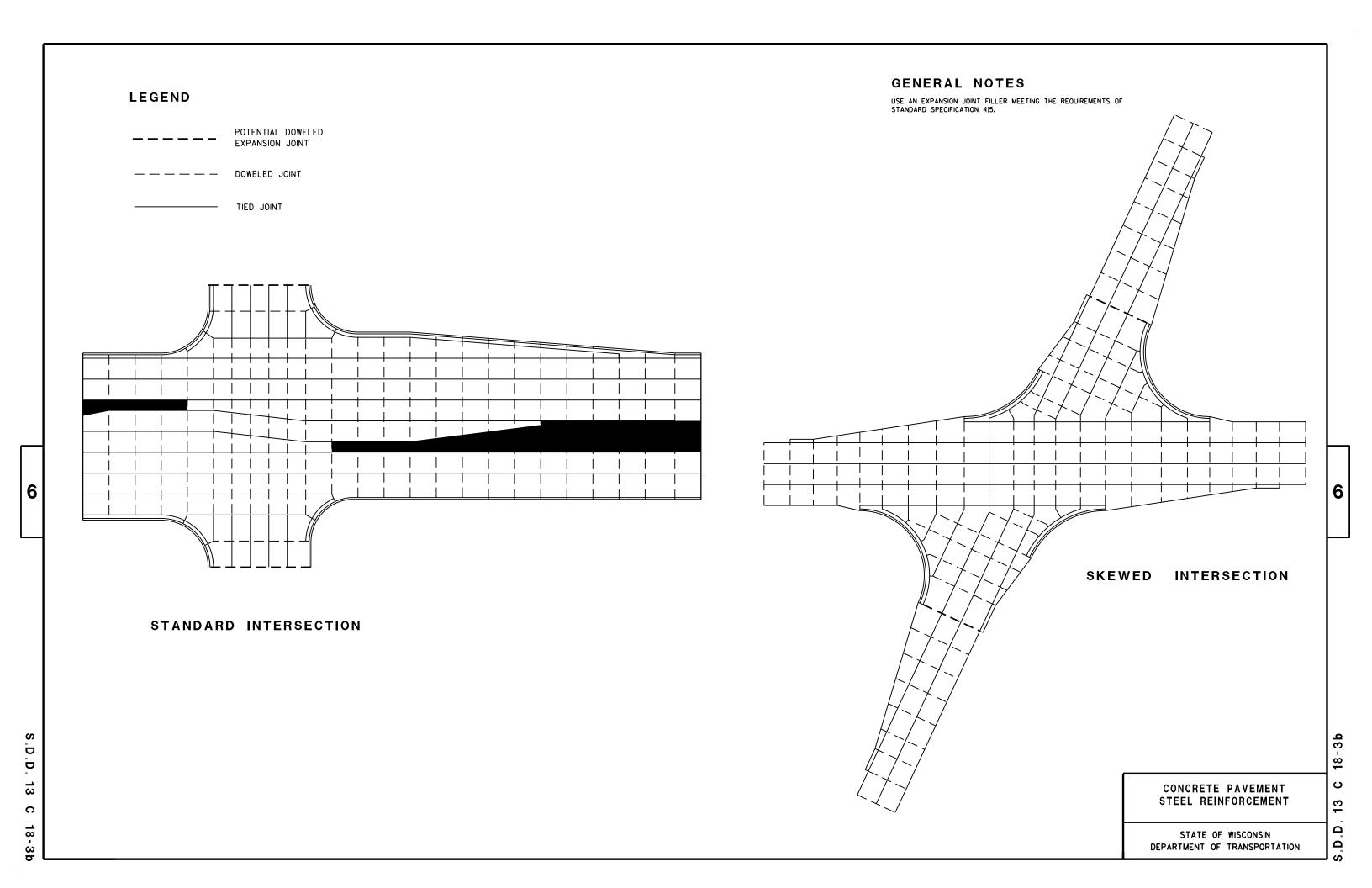
/S/ Deb Bischoff PAVEMENT POLICY & DESIGN ENGINEER

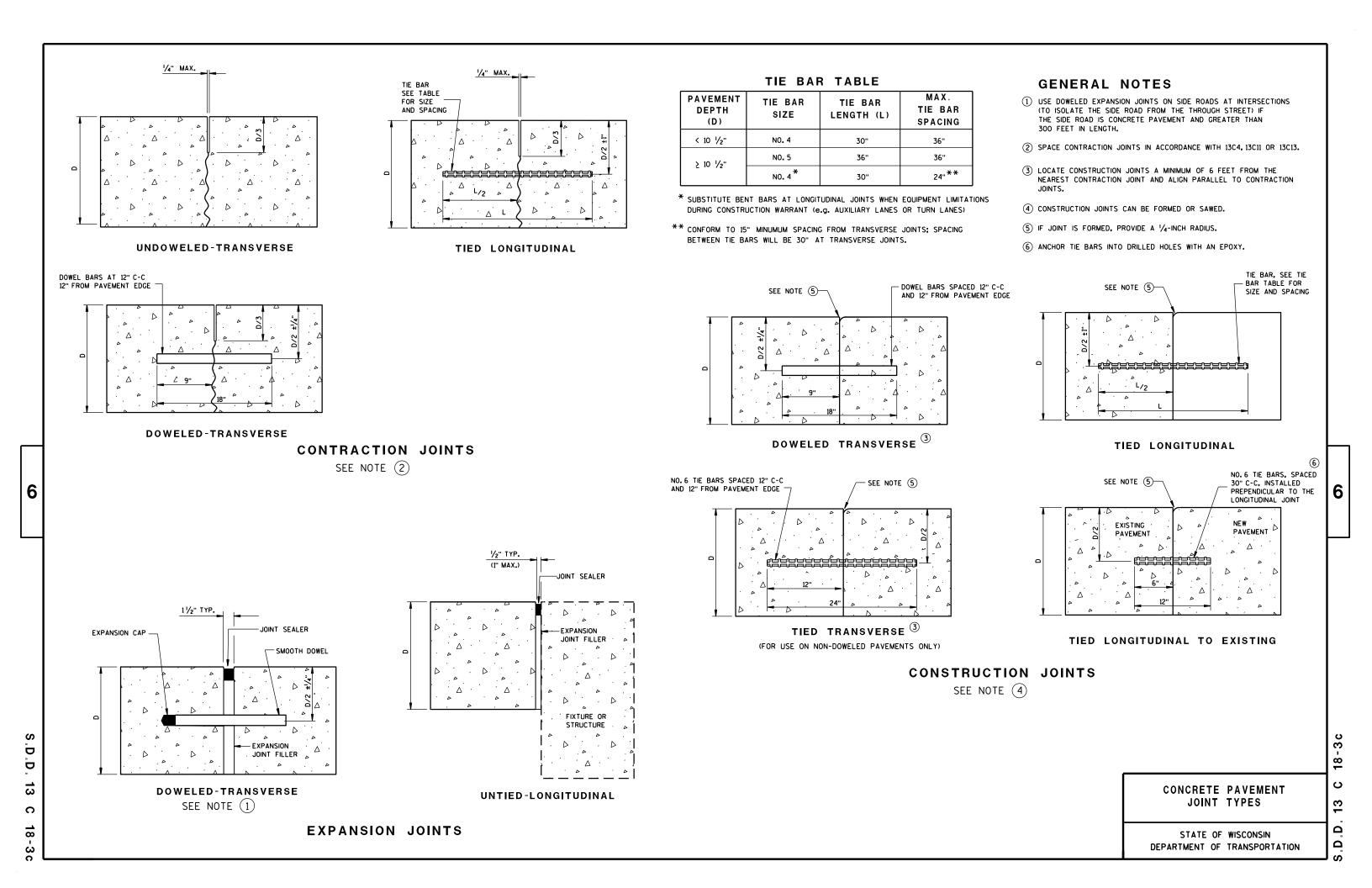
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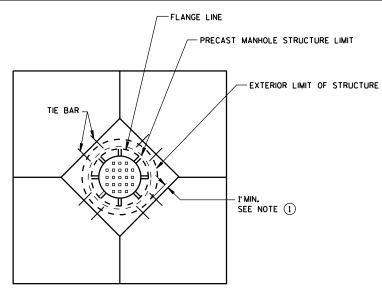
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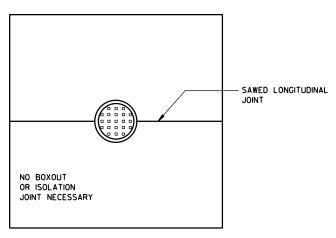




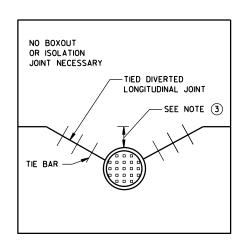




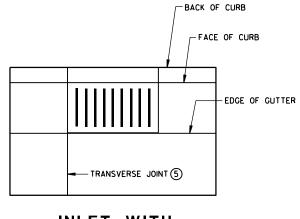
DIAGONAL MANHOLE BOXOUT FOR CONSTRUCTION JOINTS



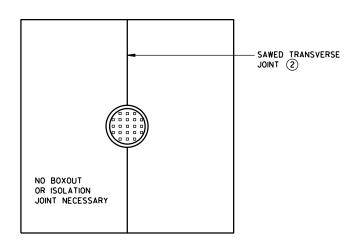
MANHOLE WITH LONGITUDINAL JOINT



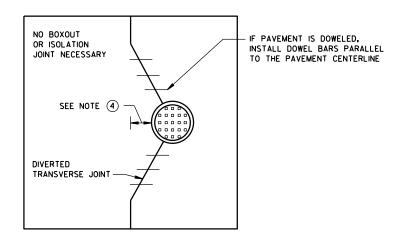
MANHOLE WITH DIVERTED LONGITUDINAL CONTRACTION JOINT



INLET WITH TRANSVERSE JOINT



MANHOLE WITH TRANSVERSE JOINT



MANHOLE WITH DIVERTED TRANSVERSE CONTRACTION JOINT

- 1 USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1-FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.
- 2 ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.
- (3) IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS. DIVERT THE LONGITUDIAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
- (4) IF DISTANCE FROM THE EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS 4 FEET OR LESS. REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
- (5) ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.

CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED DATE

/S/ Peter Kemp, P.E. PAVEMENT SUPERVISOR FHWA

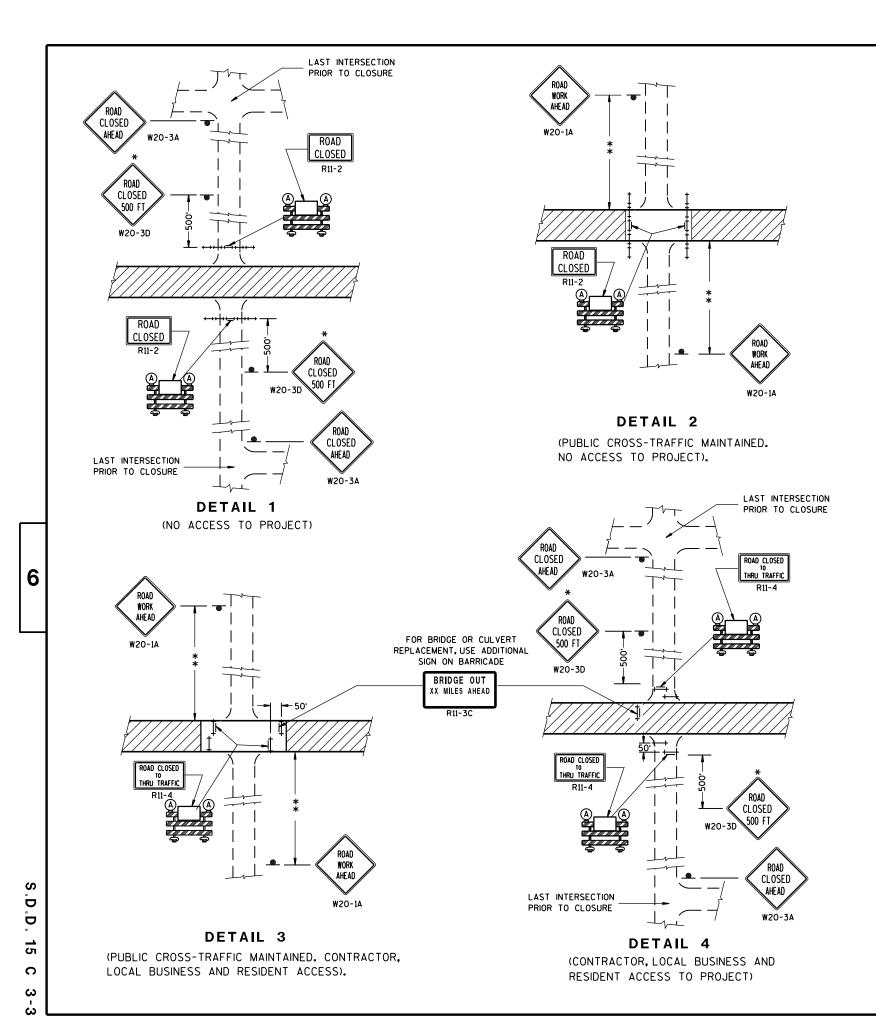
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THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

SIGN ON PERMANENT SUPPORT

TYPE III BARRICADE

TYPE III BARRICADE WITH
ATTACHED SIGN

(A) TYPE "A" WARNING LIGHT (FLASHING)

WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

Sept. 2015

DATE
STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

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THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

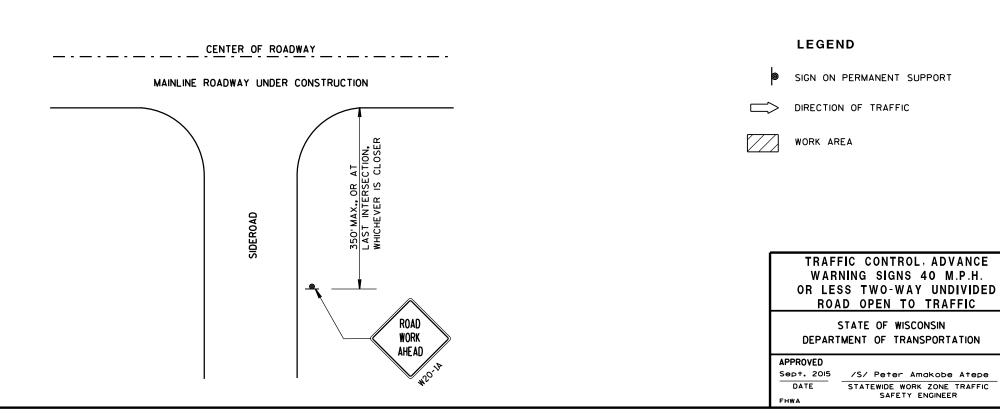
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"×36" SIGNS MAY BE USED INSTEAD OF 48"×48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

★ THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

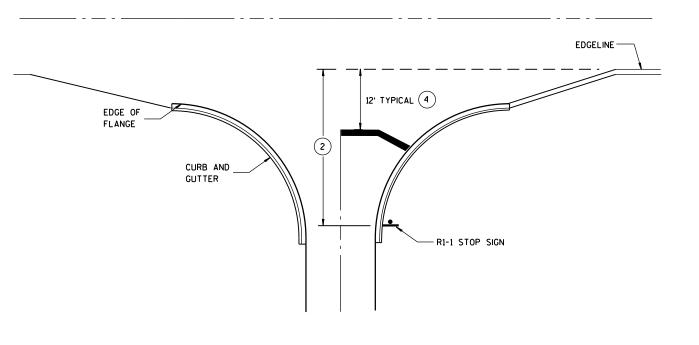


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8" CHANNELIZATION WHITE

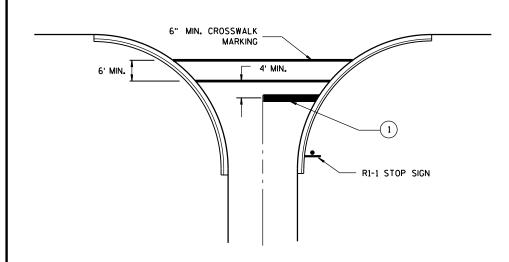
FLANGELINE (EXTENSION)

4" WHITE EDGELINE

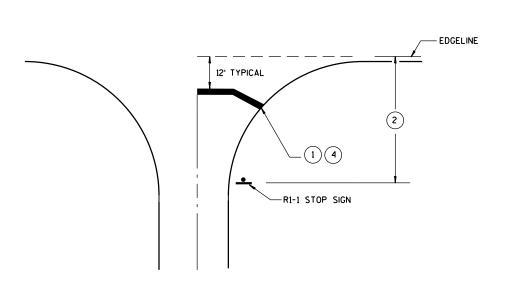
RI-1 STOP SIGN

TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

- 1 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- (2) IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
- (3) IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES. (NO CLOSER THAN 4 FEET).

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED	
4-18-2016	/S/ Matthew R. Rauch
DATE	STATE SIGNING AND MARKING ENGINEER

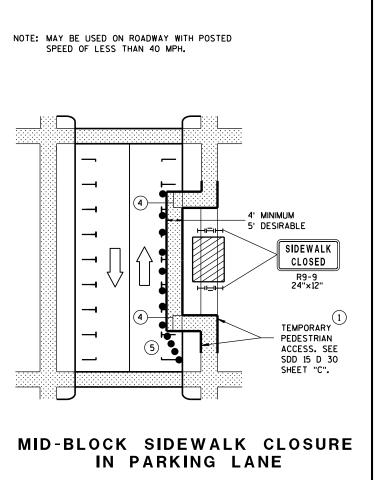
.D.D. 15 C 33-2

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NOTE: LAYOUT SAME AS ABOVE. 4' MINIMUM 5' DESIRABLE SIDEWALK CLOSED RQ-Q TEMPORARY PEDESTRIAN ACCESS. SEE SDD 15 D 30 SHEET "C". SIDEWALK DIVERSION

6

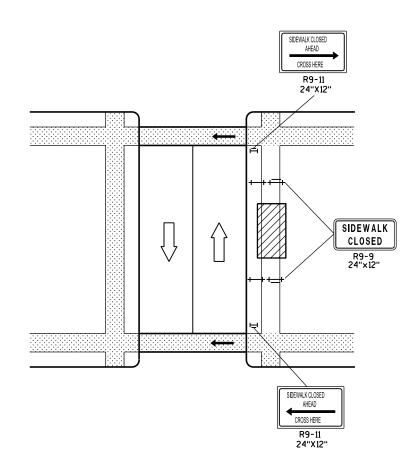
D

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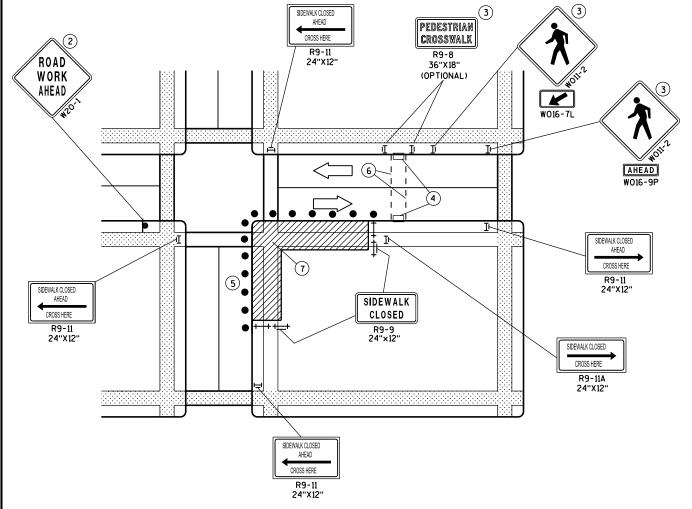
15

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MID-BLOCK SIDEWALK CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- 1) IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.
- (2) "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- (3) IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND WO11-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.
- (4) TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".
- (5) DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- (6) TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- (7) LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN

LEGEND

SIGN ON PERMANENT SUPPORT

UNDER PEDESTRIAN TRAFFIC

TRAFFIC TRAFFIC CONTOL DRUM

DIRECTION OF

WORK AREA PEDESTRIAN

CHANNELIZATION DEVICE

TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A. LOW-INTENSITY FLASHING)

TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION က 0 က Ω Ω

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PARALLEL TO CURB

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GENERAL NOTES

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION. ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- (1) CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 8D5 SHEET "E".
- (2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- 3 DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- (4) CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
- (5) CLEAR SPACE OF 48"X48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
- (6) THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
- 7 DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- (8) LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- (9) CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- (10) 5' WIDE MIN. WITH PEDSETRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.

DEPARTMENT OF TRANSPORTATION

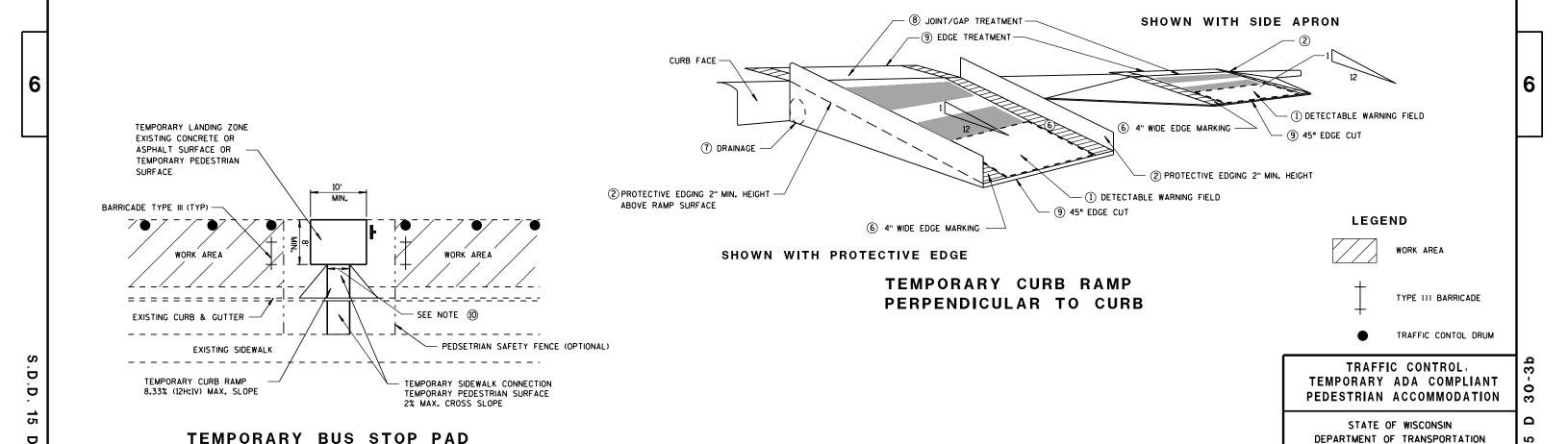
/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC

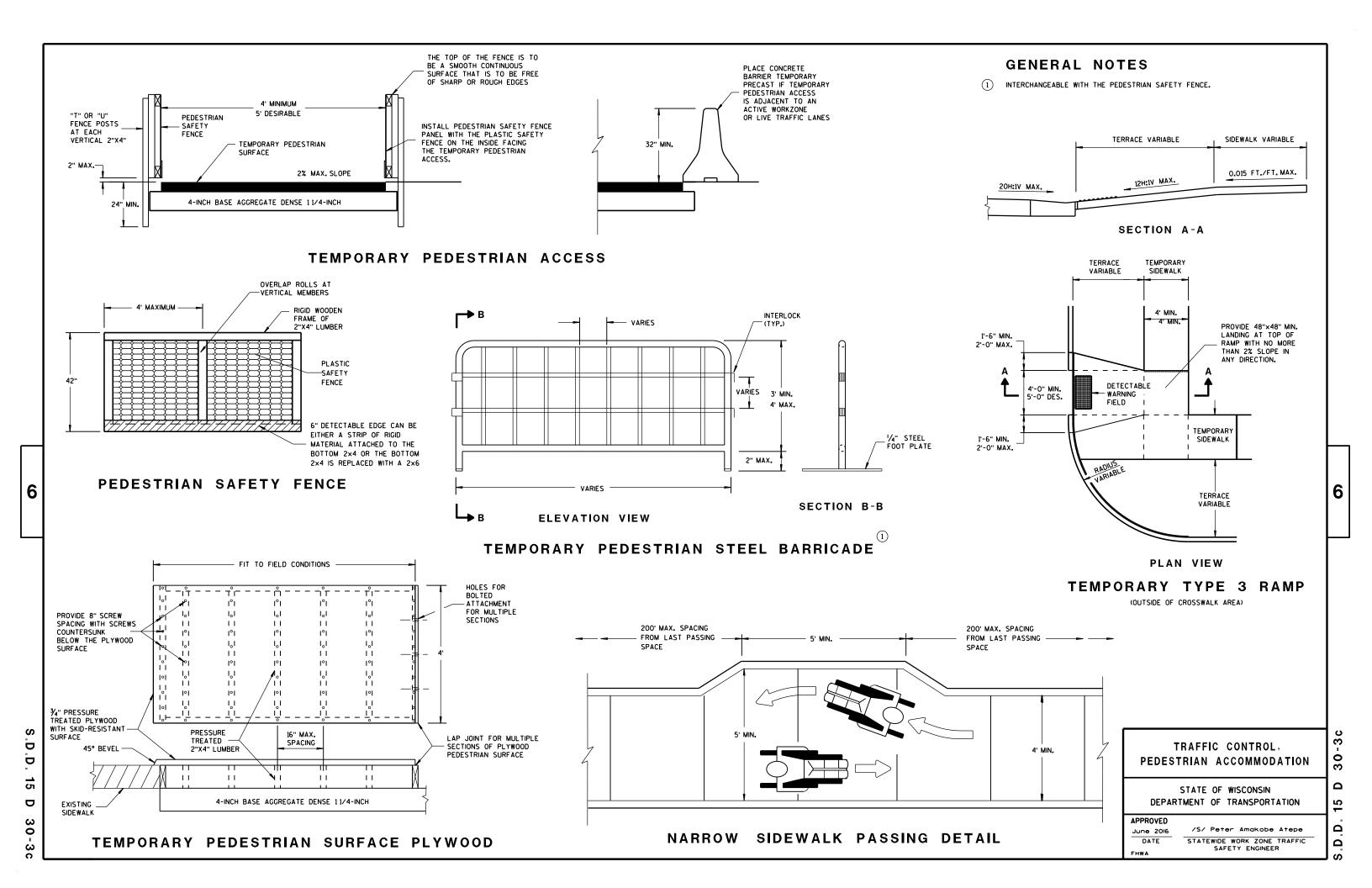
SAFETY ENGINEER

Ω

APPROVED

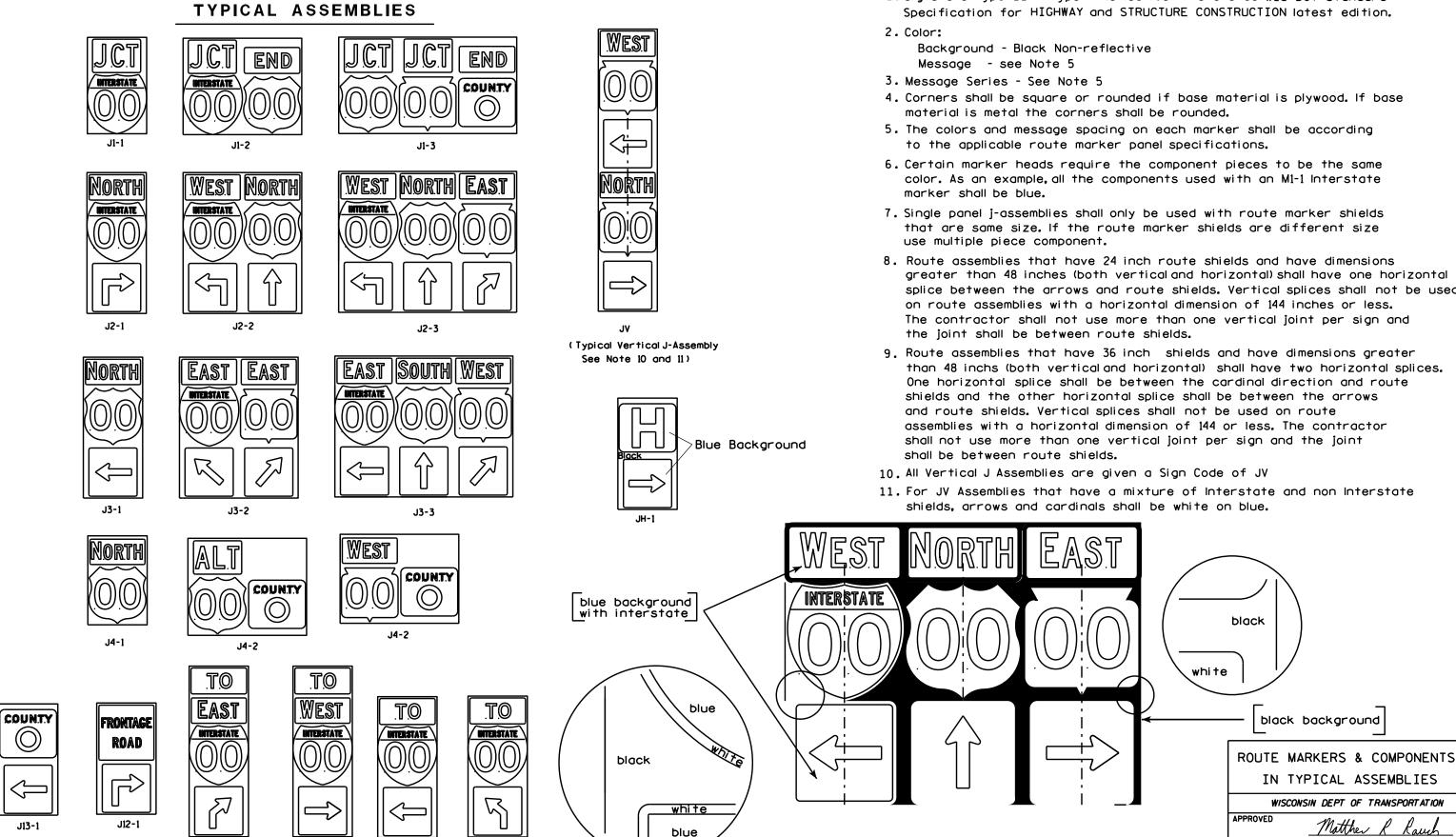
June 2016





1. Signs are Type II - Type H Reflective - reference WIS DOT Standard

areater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.



PROJECT NO:

J32-1

J22-1

J23-1

J33-1

PLOT BY: mscsja

PLATE NO. __A2-15.8

DATE 2/06/14

SHEET NO:

URBAN ARFA



RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



5'-3"(生) D^{-1} Outside Edae of Gravel

White Edgeline Location

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where

there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

PLOT BY : mscj9h

GENERAL NOTES

- 1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is $7'-3''(\pm)$ or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A2-1S) is $7'-3''(\pm)$ or $6'-3''(\pm)$ per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is $5' - 3'' (\pm)$.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (+) tolerance for mounting height is 3 inches.
- 8. Folding signs shall be mounted at a height of 5'-3'' (\pm) or as directd by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm) . The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3'' (\pm).

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

APPROVED

for State Traffic Engineer

DATE 7/23/15

PLATE NO. <u>A4-3.20</u>

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A43.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:21

COUNTY:

PLOT NAME :

PLOT SCALE: 99.237937:1.000000

WISDOT/CADDS SHEET 42



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

WISDOT/CADDS SHEET 42

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3'' (±) or 6'-3'' (±) per urban or rural detail respectively.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).
- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- *** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

URBAN AREA RURAL AREA (See Note 3) 2'Min - 4'Max (See Note 6) ₩E# FF# 6'-3"(±) 6'-3"(±) 7'-3"(±) ** Curb ****\ Flowline D **7000** White Edgeline D 11 White Edgeline, Location Outside Edae Location

2'Min - 4'Max (See Note 6) 6'-3"(±) Curb Flowline. -11

48" DIAMOND WARNING SIGN

HWY:

_ 26" 5 ' - 3 "(±) White Edgeline Location Outside Edge of Gravel 48" DIAMOND WARNING SIGN

COUNTY:

Outside Edge

of Gravel

	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRED		
	L	E	
* * *	Greater than 48" Less than 60"	12"	
	60" to 120"	L/5	l

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN (FOUR POSTS REQUIRE	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

of Gravel

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

Matther

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:23

PLOT SCALE: 107.021305:1.000000

WISDOT/CADDS SHEET 42

PLOT NAME :

PLOT BY: mscj9h

WISCONSIN DEPT OF TRANSPORTATION APPROVED

For State Traffic Engineer

PLATE NO. 44-4.14 DATE 7/23/15



Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Nather R Raw
For State Traffic Engineer

DATE <u>8/11/16</u>

PLATE NO. <u>44-8.8</u>

PROJECT NO:

FILE NAME : C:\CAFfiles\Projects\tr stdplote\A48 DCN

PLOT DATE . 11-416-2016 11:35

PINT RY * \$\$ nintuser \$\$

SHEET NO:

| | |



PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

For State Traffic Engineer



1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Metric equivalent for this sign is:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 1/8	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72

COUNTY:

STANDARD SIGN G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

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AP

for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\G202A.DGN

HWY:

PROJECT NO:

PLOT DATE: 30-SEP-2009 09:31

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE : 5.561773:1.000000

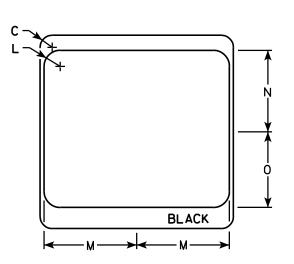
5.561773:1.000000 WISDOT/CADDS SHEET 42

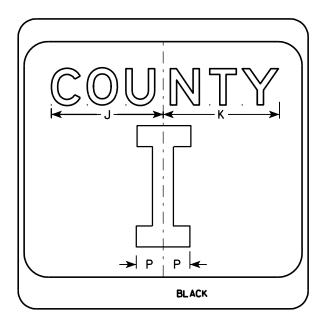
- 1. Sign is Type II see Note 7 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

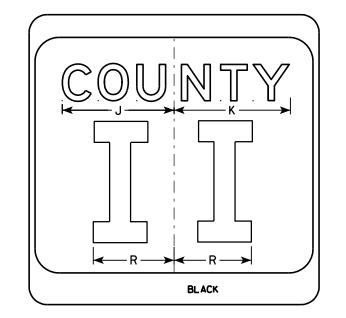
Background - White & Black - See Note 7 Message - Black

- 3. Message Series see Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Message Series E for 1 letter. Message Series D for 2 letters unless message is too big then Series C. Message Series C for 3 letters unless message is too big then Series B.
- 6. Substitute appropriate letters & optically center to achieve proper balance.
- 7. Permanent Signs

Background - Type H Reflective Detour or temporary Signs Background - Reflective







SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 %	2	11 1/2	10 1/8	9 %	2 1/4		6 %									4.0
3	36		2 1/4			16	4	7 5/8	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 %		10									9.0
5	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
																			_								

COUNTY:

CTH MARKER M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

FerState Traffic Engineer PLATE NO. M1-5A.8 DATE 9/27/11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M15A.DGN

PROJECT NO:

BLACK

HWY:

M1-5A

PLOT DATE: 29-SEP-2011 11:25

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 5.959043:1.000000

WISDOT/CADDS SHEET 42

- 1. Sign is Type II See Note 6 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White & Black - See Note 6 Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
- 6. Permanent Signs Background - Type H Reflective Detour or temporary Signs Background - Reflective

BLACK	↑
Metric equivalent for this sign is:	

HWY:

SIZE 600 mm X 600 mm 900 mm X 900 mm 900 mm X 900 mm 900 mm X 900 mm

PROJECT NO:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 %	11 1/2	1	1 1/8	11 1/4	21 1/8											4.0	. 36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 %	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 1/8	16 %	33											9.0	. 81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	. 81
\equiv																												

COUNTY:

STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 3/20/02 PLATE NO. M1-6.9

SHEET NO:

FILE NAME : C:\Users\Projects\tr_stdplate\M16.DGN

PLOT DATE: 13-OCT-2005 14:55

PLOT BY : DITJPH

PLOT NAME :

PLOT SCALE : 6.715871:1.000000

WISDOT/CADDS SHEET 42

- 1. Sign is Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M2-1 Background White

Message - Black

MB2-1 Background - Blue

Message - White

MK2-1 Background - Green

Message - White

MM2-1 Background - White

Message - Green

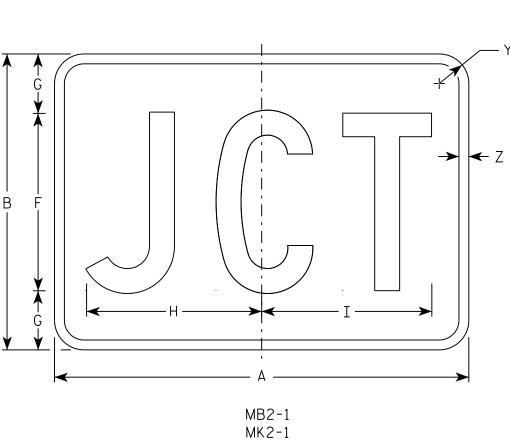
MN2-1 Background - Brown Message - White

MP2-1 Background - White

Message - Blue

MR2-1 Background - Brown

Message - Yellow



M2-1

MM2-1

MP2-1

HWY:

MN2-1

MR2-1

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 1/8	8 %																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 1/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 1/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 1/8	12 3/8																1 1/2	1/2	4.40

COUNTY:

В

STANDARD SIGN

M2 - 1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R Rauch f_{or} State Traffic Engineer

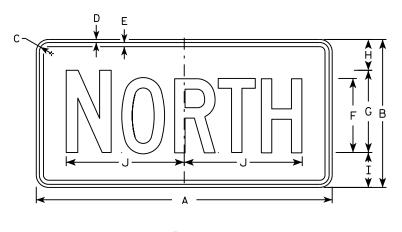
DATE 10/15/15

PLATE NO. M2-1.12 Ε

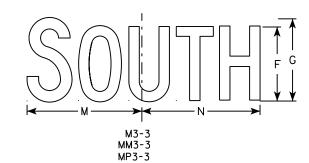
SHEET NO:

PLOT SCALE • 4 864603•1 000000

PROJECT NO:

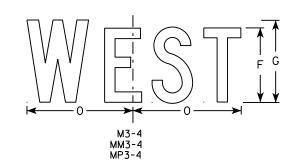




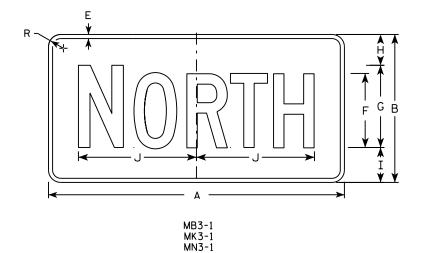


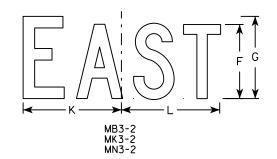
M3-2 MM3-2

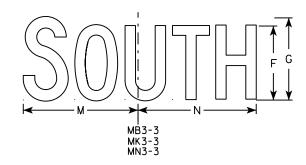
MP3-2

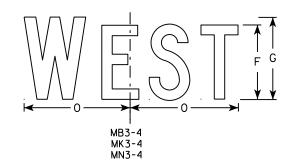


HWY:









NOTES

- 1. All Signs Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

5. M3-1 thru M3-4 Background - White Message - Black

MB3-1 thru MB3-4 Background - Blue

Message - White

MK3-1 thru MK3-4 Background - Green

Message - White

MM3-1 thru MM3-4 Background - White

Message - Green

MN3-1 thru MN3-4 Background - Brown

Message - White

MP3-1 thru MP3-4 Background - White

Message - Blue

6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1 1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 1/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

COUNTY:

STANDARD SIGNS M3-1 thur M3-4 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew Rauge State Traffic Engineer

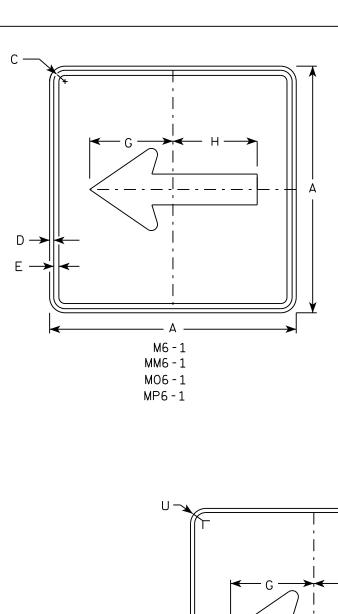
DATE 10/15/15 PLATE NO. M3-1.14

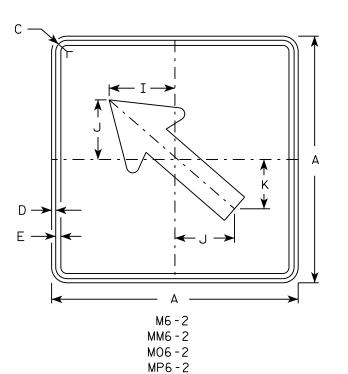
Ε

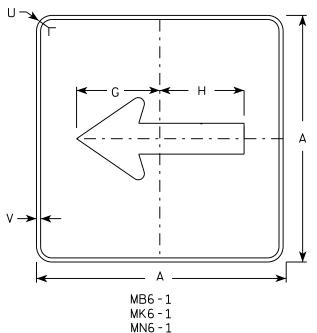
SHEET NO:

-DFC-2015 17:54 PIOT RY: \$\$ DIOTUSER \$\$ PIOT NAME: PIOT SCAI

PROJECT NO:

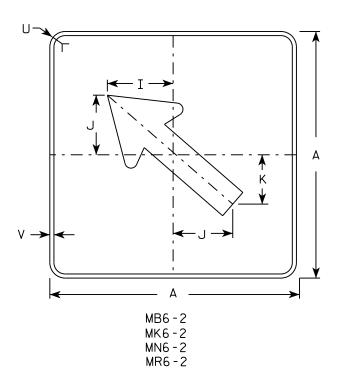






MR6-1

HWY:



NOTES

- 1. Signs are Type II Type H except as Shown
- 2. Color:

Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background White

Message - Black

MB6-1 and MB6-2 Background - Blue

Message - White

MK6-1 and MK6-2 Background - Green

Message - White

MM6-1 and MM6-2 Background - White

Message - Green

MN6-1 and MN6-2 Background - Brown

Message - White

M06-1 and M06-2 Background - Orange - Type F Reflective

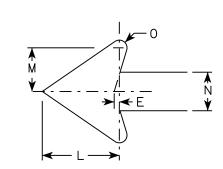
Message - Black

MP6-1 and MP6-2 Background - White

Message - Blue

MR6-1 and MR6-2 Background - Brown

Message - Yellow



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	٥	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1 1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 %	5	4 1/4	5 1/4	3	2 %	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew & Rawl For State Traffic Engineer

Ε

DATE 10/15/15 PLATE NO. M6-1.15

SHEET NO:

FILE NAME · C·\CAFfiles\Projects\tr stdplote\M61 DCN

PROJECT NO:

PLOT DATE . 01-DEC-2015 17:57

PIOT RY . \$\$ plotuser \$\$ PIOT NAMF :

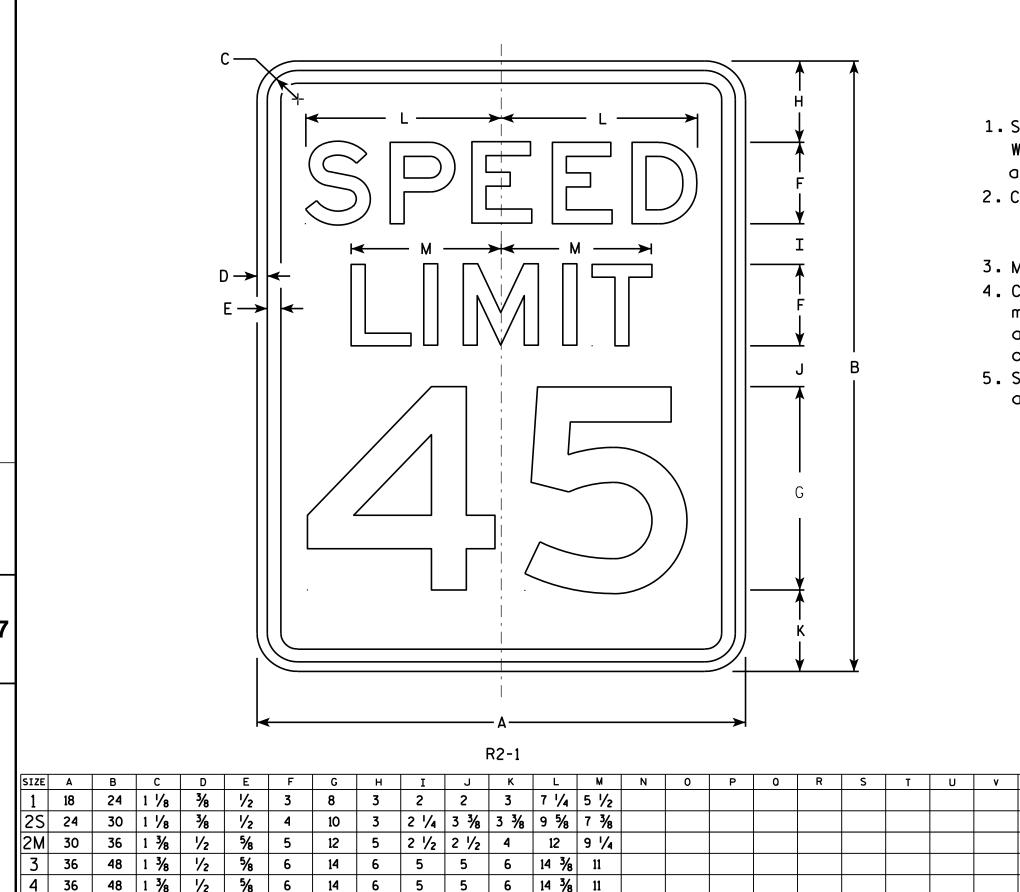
PLOT SCALE . 11 675051.1 000000

NOTES 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. 2. Color: Background - Red Message - White 3. Message Series - C R1-1 SIZE A STANDARD SIGN 30 5/8 10 12 1/2 45° 12 3/4 5.18 2S 30 5/8 12 1/2 45° 12 3/4 10 5.18 R1-1 2M 36 3/4 12 15 45° 15 % 7.46 3/4 15 3/8 12 45° 36 15 7.46 WISCONSIN DEPT OF TRANSPORTATION 45° 20 1/2 48 16 20 13.25 APPROVED Matthew & Kauch 5 48 16 20 45° 20 1/2 13.25 3/8 7 3/4 45° 7 3/4 1.86 18 6 For State Traffic Engineer 12 1/4 4 45° 5 1/8 0.78 DATE <u>11/12/15</u> PLATE NO. _____R1-1.13 COUNTY: SHEET NO: PROJECT NO: HWY: PLOT SCALE • 4 378143•1 000000

FILE NAME · C·\CAFfiles\Projects\tr stdplote\R11 DGN

PLOT DATE . 01-DEC-2015 18:07

PINT RY . \$\$ plotuser \$\$ PINT NAMF :



4 1/2 6 3/4 6 3/4 19 1/4 14 5/8

COUNTY:

20

HWY:

6

NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal. the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

3.0

5.0

7.5

12.0

12.0

20.0

STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION APPROVED

Matther R Raus For State Traffic Engineer PLATE NO. R2-1.13

DATE <u>5/26/1</u>0

SHEET NO:

2 1/4

60

5

48

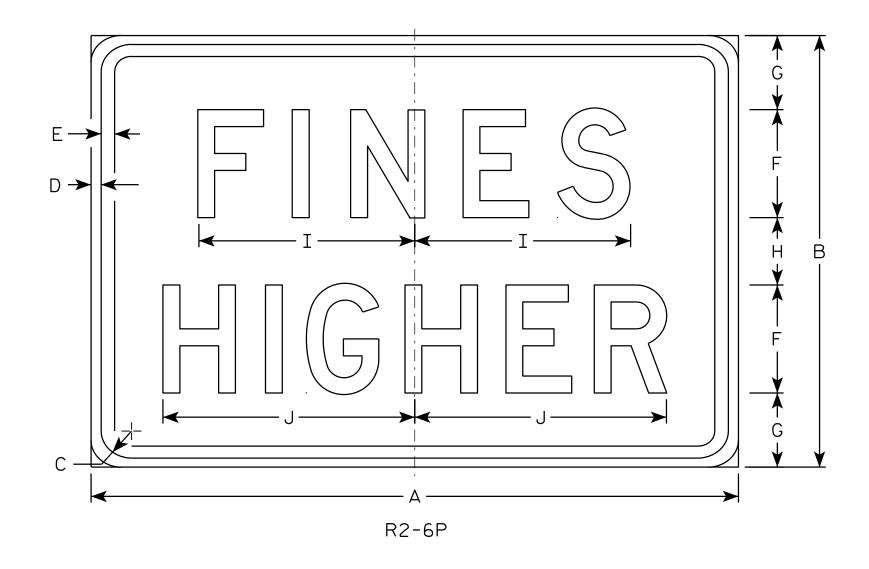
PROJECT NO:

PLOT NAME :

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Α	В	С	D	Ε	F	G	н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
24	18	1 1/8	3∕8	3/8	4	3 1/2	3	7 %	9 1/4																	3.0
24	18	1 1/8	3/8	3/8	4	3 1/2	3	7 %	9 1/4																	3.0
36	24	1 1/8	3∕8	1/2	6	4 1/8	3 3/4	12	14																	6.0
36	24	1 1/8	3∕8	1/2	6	4 1/8	3 3/4	12	14																	6.0
48	36	1 3/8	1/2	5/8	8	7	6	15 1/8	19																	12.0
	24 36 36	24 18 24 18 36 24 36 24	24 18 1 ½8 24 18 1 ½8 36 24 1 ½8 36 24 1 ½8	24 18 1 ½ 3/8 24 18 1 ½ 3/8 36 24 1 ½ 3/8 36 24 1 ½ 3/8	24 18 1 ½ 3½ 3½ 24 18 1 ½ 3½ 3½ 36 24 1 ½ 3½ ½ 36 24 1 ½ 3½ ½	24 18 1 ½ 3½ 3½ 4 24 18 1 ½ 3½ 3½ 4 36 24 1 ½ 3½ ½ 6 36 24 1 ½ 3½ ½ 6	24 18 1 ½ 3/8 3/8 4 3 ½ 24 18 1 ½ 3/8 3/8 4 3 ½ 36 24 1 ½ 3/8 ½ 6 4 ½ 36 24 1 ½ 3/8 ½ 6 4 ½	24 18 1 ½ 3/8 3/8 4 3 ½ 3 24 18 1 ½ 3/8 3/8 4 3 ½ 3 36 24 1 ½ 3/8 ½ 6 4 ½ 3 ¾ 36 24 1 ½ 3/8 ½ 6 4 ½ 3 ¾	24 18 1 ½ 3½ 3½ 4 3 ½ 3 7 ½ 24 18 1 ½ 3½ 3½ 3 7 ½ 36 24 1 ½ 3½ 3½ 4 3 ½ 3 7 ½ 36 24 1 ½ 3½ ½ 6 4 ½ 3 ¾ 12 36 24 1 ½ 3½ ½ 6 4 ½ 3 ¾ 12	24 18 1 ½ 3½ 3½ 4 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 4 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ ½ 6 4 ½ 3 ¾ 12 14 36 24 1 ½ 3½ ½ 6 4 ½ 3 ¾ 12 14	24 18 1 ½ 3½ 3 7 ½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 8 4 3 ½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 6 4 ½ 3 ¾ 12 14 36 24 1 ½ 3½ 6 4 ½ 3 ¾ 12 14	24 18 1 1/8 3/8 3/8 4 3 1/2 3 7 5/8 9 1/4 24 18 1 1/8 3/8 3/8 4 3 1/2 3 7 5/8 9 1/4 36 24 1 1/8 3/8 1/2 6 4 1/8 3 3/4 12 14 36 24 1 1/8 3/8 1/2 6 4 1/8 3 3/4 12 14	24 18 1 1/8 3/8 3/8 4 3 1/2 3 7 5/8 9 1/4 24 18 1 1/8 3/8 3/8 4 3 1/2 3 7 5/8 9 1/4 36 24 1 1/8 3/8 1/2 6 4 1/8 3 3/4 12 14 36 24 1 1/8 3/8 1/2 6 4 1/8 3 3/4 12 14	24 18 1 ½ 3½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 4 3 ½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 6 4 ½ 3 ¾ 12 14 36 24 1 ½ 3½ 6 4 ½ 3 ¾ 12 14	24 18 1 ½ 3½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 4 3 ½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 4 3 ½ 12 14 36 24 1 ½ 3½ 3½ 6 4 ½ 3 ¾ 12 14	24 18 1 ½ 3½ 4 3 ½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 4 3 ½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 4 3 ½ 12 14 36 24 1 ½ 3½ 5 6 4 ½ 3 ¾ 12 14	24 18 1 1/8 3/8 3/8 4 3 1/2 3 7 5/8 9 1/4 24 18 1 1/8 3/8 3/8 4 3 1/2 3 7 5/8 9 1/4 36 24 1 1/8 3/8 1/2 6 4 1/8 3 3/4 12 14 36 24 1 1/8 3/8 1/2 6 4 1/8 3 3/4 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3½ 9 ¼ 36 24 1 ½ 3½ 3½ 12 14 36 24 1 ½ 3½ 1½ 12 14 36 24 1 ½ 3½ 1½ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 3½ 12 14 36 24 1 ½ 3½ 1½ 12 14 36 24 1 ½ 3½ 1½ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 6 4 ½ 3 ¾ 12 14 36 24 1 ½ 3½ ½ 6 4 ½ 3 ¾ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 4 3 ¾ 12 14 36 24 1 ½ 3½ 1/2 6 4 ½ 3 ¾ 12 14 36 24 1 ½ 3½ 1/2 6 4 ½ 3 ¾ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 3½ 12 14 36 24 1 ½ 3½ ½ 6 4 ½ 3 ¾ 12 14 36 24 1 ½ 3½ ½ 6 4 ½ 3 ¾ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 3½ 12 14 36 24 1 ½ 3½ 1/2 6 4 ½ 3 ¾ 12 14 36 24 1 ½ 3½ 1/2 6 4 ½ 3 ¾ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 24 18 1 ½ 3½ 3 7 ½ 9 ¼ 36 24 1 ½ 3½ 3 12 14 36 24 1 ½ 3½ 3¾ 12 14 36 24 1 ½ 3½ 3¾ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ½ 24 18 1 ½ 3½ 3 7 ½ 9 ½ 24 18 1 ½ 3½ 3 7 ½ 9 ½ 36 24 1 ½ 3½ 3 12 14 36 24 1 ½ 3½ 3½ 12 14 36 24 1 ½ 3½ 3½ 12 14	24 18 1 ½ 3½ 3 7 ½ 9 ½ 24 18 1 ½ 3½ 3 7 ½ 9 ½ 24 18 1 ½ 3½ 3 7 ½ 9 ½ 36 24 1 ½ 3½ 3 12 14 36 24 1 ½ 3½ 3½ 12 14 36 24 1 ½ 3½ 3½ 12 14

COUNTY:

STANDARD SIGN R2-6P

WISCONSIN DEPT OF TRANSPORTATION

ROVED Matther R Rauch
For State Traffic Engineer

DATE 12/20/10 PLATE NO. R2-6P.2

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R26P.DGN

PROJECT NO:

HWY:

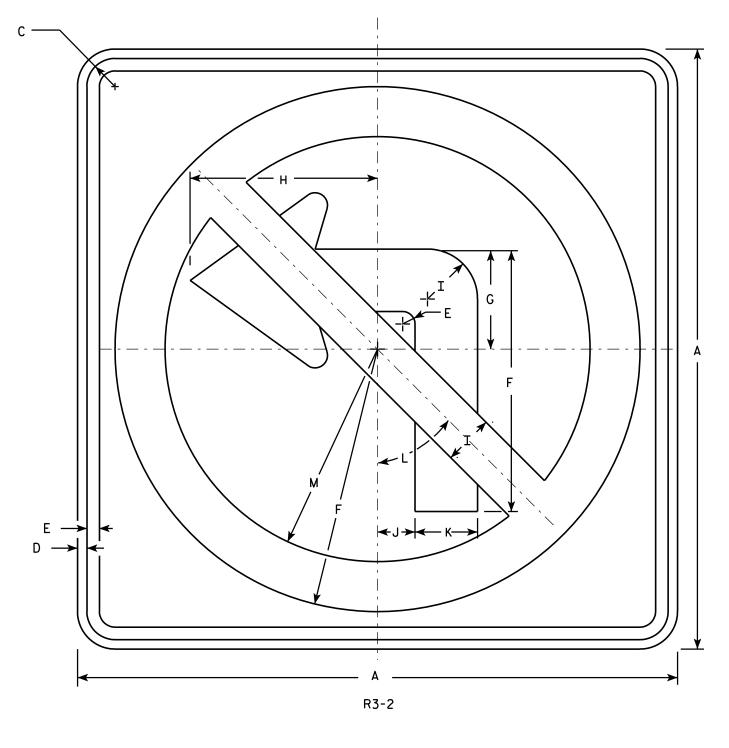
PLOT DATE: 20-DEC-2010 14:18

PLOT BY: dotsja

PLOT NAME :

PLOT SCALE: 5.338308:1.000000

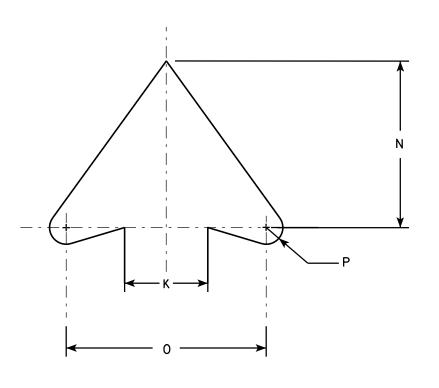
WISDOT/CADDS SHEET 42



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	₩	X	Y	Z	Area sq. ft.
1	24		1 1/8	3⁄8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	9	1/2											4.0
2M	36		1 1/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		1 1/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		1 1/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

COUNTY:

STANDARD SIGN R3-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

 $f_{\it or}$ State Traffic Engineer

DATE 12/08/10

PLATE NO. R3-2.10

SHEET NO:

HWY:

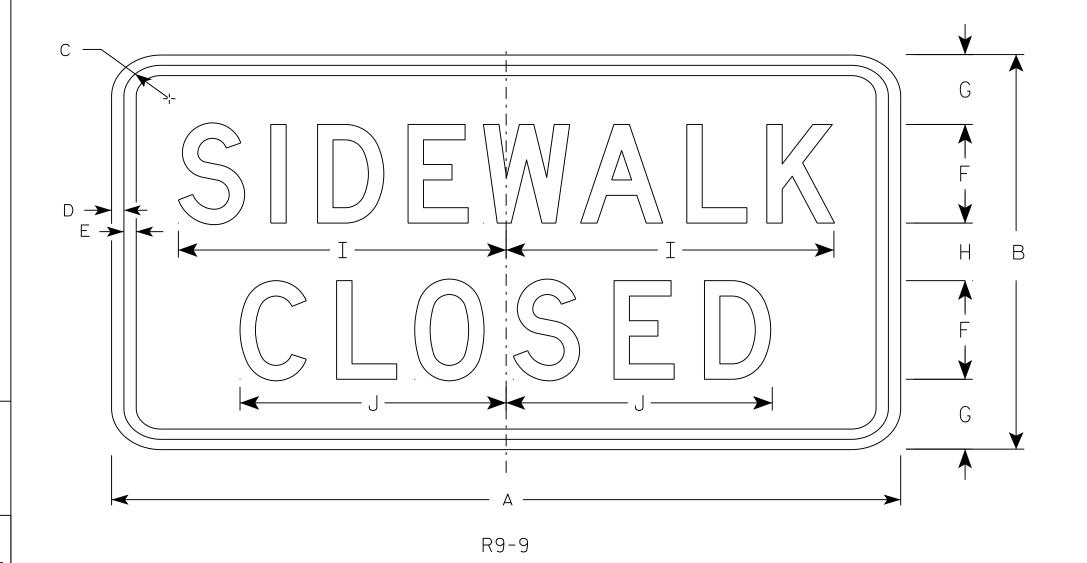
PROJECT NO:

PLOT NAME :

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



SIZE A 2S 24 1 3/4 1/2 2 1/8 1 3/4 10 1/2 12 3 8 1/8 2.0 24 1 3/4 1/2 2 1/8 1 3/4 8 1/8 12 10 2.0 1 3/4 3 1/2 30 18 1/2 1/2 3 | 12 1/2 | 10 1/4 3.75

COUNTY:

STANDARD SIGN R9-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Marther R Ray

DATE <u>8/11/16</u>

SHEET NO: R9-9.6

Ε

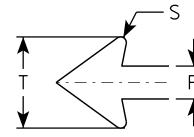
HWY:

PROJECT NO:

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C except Size 1 is Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



← P →	F V	<u> </u>

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	v	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 %	3 ½	9 1/4	6 %	5 1/8		1	1/8	2 3/4							2.0
2M	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 %	3 1/2	9 1/4	6 %	5 1/8		1	1/8	2 3/4							2.0
3	48	30	2 3/4	3/4	3/4	4	2 1/2	3 1/2	20 ½	1 1/4	3	16 3/8	7 1/4	18 ½	14	11 1/8		2	3∕8	5 1/2							10.0
4																											
5																											

STANDARD SIGN R9-11

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

OVED Matthew R Rouse

DATE 3/10/16

10/16 PLATE NO. R9-11.3

SHEET NO:

HWY:

COUNTY:

PLOT DATE: 10-MAR-2016 14:57

PLOT NAME :

PLOT BY: mscsja

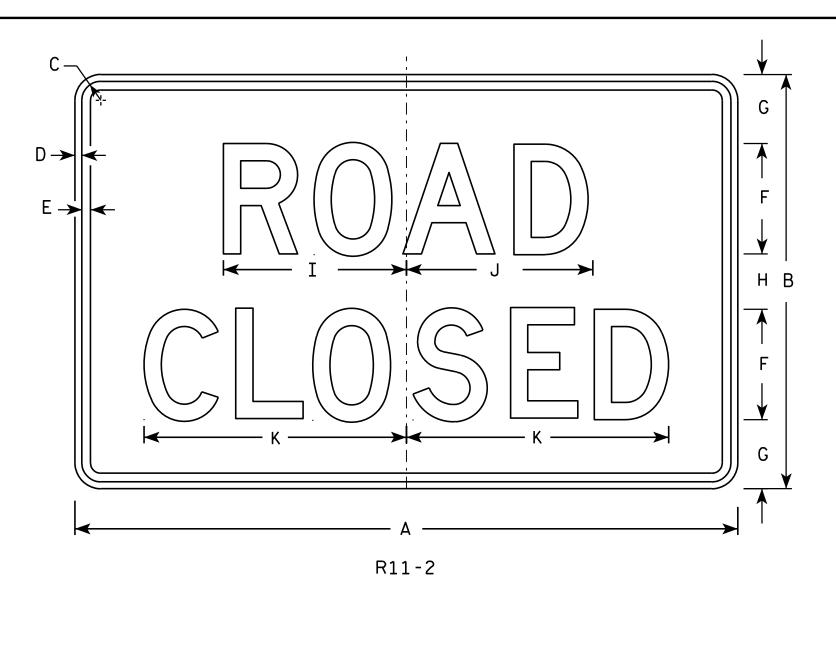
PLOT SCALE: 5.837529:1.000000

WISDOT/CADDS SHEET 42

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\R911.DGN

PROJECT NO:

 $D \rightarrow$

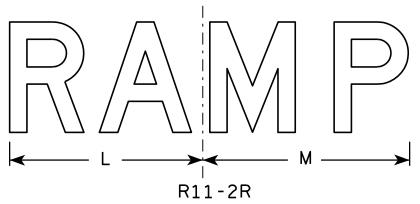


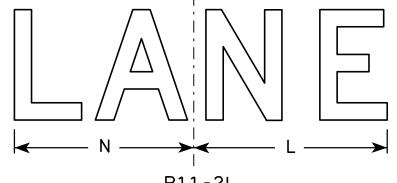
<u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Modify the message as required.





R	1	1	-	2	L

PLOT NAME :

SIZ	Έ	A	В	С	D	Ε	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																												
2	S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
21	I	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 ½	19	14	15	13													10.0
3		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
4		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
5		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 ½	19	14	15	13													10.0

COUNTY:

STANDARD SIGN R11-2

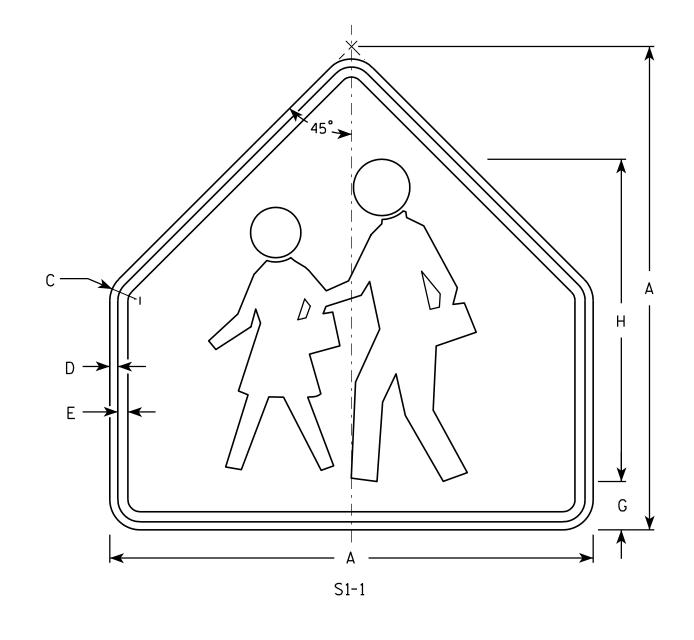
WISCONSIN DEPT OF TRANSPORTATION

DATE 4/1/11 PLATE NO. R11-2.10

SHEET NO:

HWY:

PROJECT NO:



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow-Green Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	Т	U	٧	W	X	Y	Z	Areg sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2	36		1 %	5/8	3/4		3 1/2	24																			6.75
3	36		1 %	5/8	3/4		3 1/2	24																			6.75
4	48		2 1/4	₹4	1		4 3/4	32																			12
5																											

COUNTY:

STANDARD SIGN S1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Ter State Traffic Engineer

DATE 6/30/05 PLATE NO. S1-1.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\S11.DGN

PROJECT NO:

HWY:

PLOT DATE: 26-MAY-2010 16:12

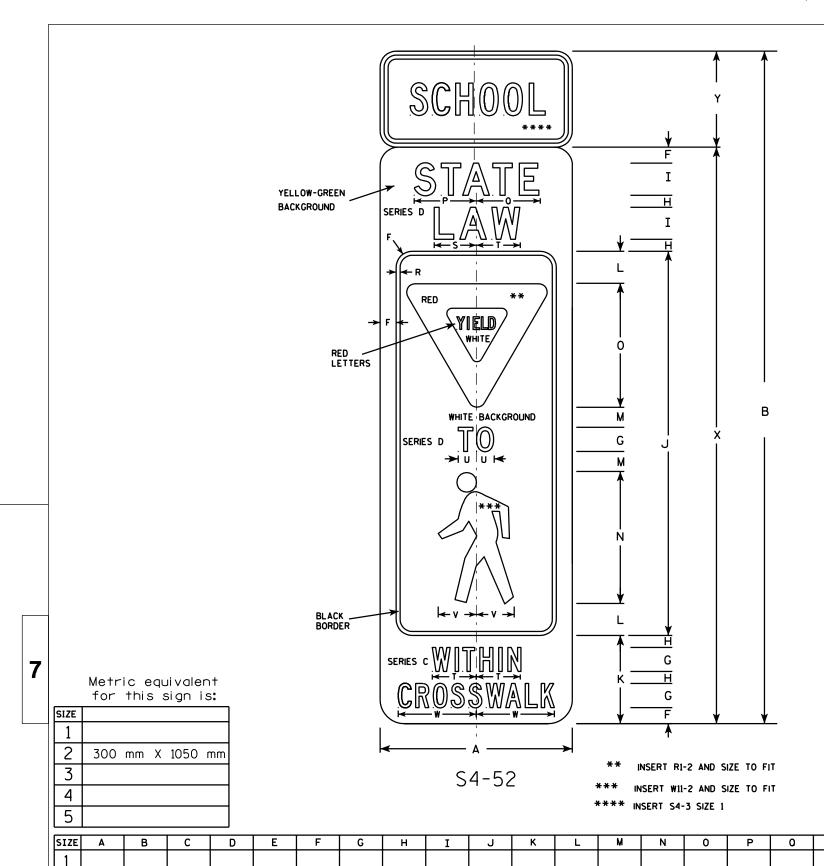
PLOT BY : ditjph

PLOT NAME :

PLOT SCALE : 5.959043:1.000000

959043:1.000000 WISDOT/CADDS SHEET 42





3/4

2 D

5 1/2

24

1 1/4

1 1/2C

NOTES

- 1. Sign is Type II- reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - YELLOW-GREEN (TYPE F REFLECTIVE)

Message - BLACK

YIELD SYMBOL - RED ON WHITE (TYPE F REFLECTIVE)

PED SYMBOL - BLACK ON WHITE (TYPE F REFLECTIVE)

- 3. Message Series AS SHOWN AND IN TABLE
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

STANDARD SIGN S4-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED MA // A A A

For State Traffic Engineer

DATE 6/14/05 PLATE NO. S4-52.1

SHEET NO:

8 1/4 7 3/4 3 7/8 4 1/4 2 5/8 2 3/4 1 1/8 2 3/8 4 7/8 36 6 3.5 0.31

1 1/2

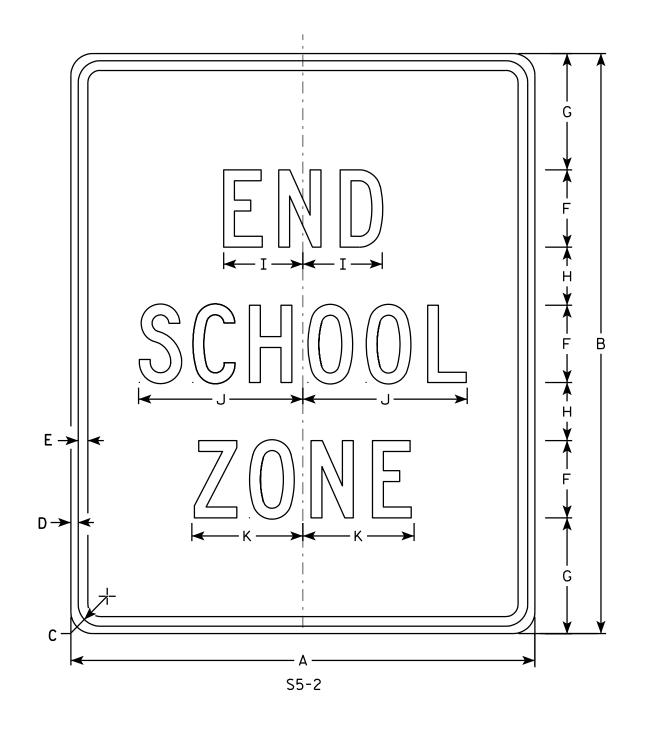
42

12

3

4

5



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

3/8 1/2 4 1/8 8 1/2 5 3/4 24 30 | 1 1/8 5.0 3 36 42 | 1 1 1 1 1 1 1 1 1 1 1 1 1 5/8 3/4 6 1/8 12 5/8 8 5/8 10.5 4 5

COUNTY:

STANDARD SIGN S5-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED For State Traffic Engineer

DATE 3/8/11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\S52.DGN

PROJECT NO:

HWY:

PLOT DATE: 08-MAR-2011 08:54

PLOT NAME :

PLOT BY: mscj9h

PLOT SCALE: 4.965868:1.000000

WISDOT/CADDS SHEET 42

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow-Green Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. S16-7R are the same as S16-7L except the arrow is reversed along the vertical centerline.

E —	
C H	
<u></u>	>
S16-7L	

SIZE	Α	В	С	D	Ε	F	G	Н	I	7	K	L	M	N	0	Р	0	R	S	Т	U	٧	W	X	Y	Z	Areg sq. ft.
1	24	12	3/8	3/8	1 1/8	3	30°	5 3/4	4	1/2	7																2.0
25	30	18	3/8	1/2	1 1/8	4 ½	30°	8 ½	6	5/8	10 1/4																3.75
2M	30	18	3/8	1/2	1 1/8	4 ½	30°	8 ½	6	5/8	10 1/4																3.75
3	30	18	3/8	1/2	1 1/8	4 1/2	30°	8 ½	6	5/8	10 1/4																3.75
4	48	24	1/2	5/8	1 3/8	6	30°	11 1/2	8	1	14																8.0
5																											

STANDARD SIGN S16-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew & Rauch

For State Traffic Engineer

DATE 7/22/13 PLATE NO. S16-7.1

SHEET NO:

HWY:

COUNTY:

PLOT BY : mscj9h

PLOT NAME :

PLOT SCALE : 3.969517:1.000000

WISDOT/CADDS SHEET 42

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\S167.dgn

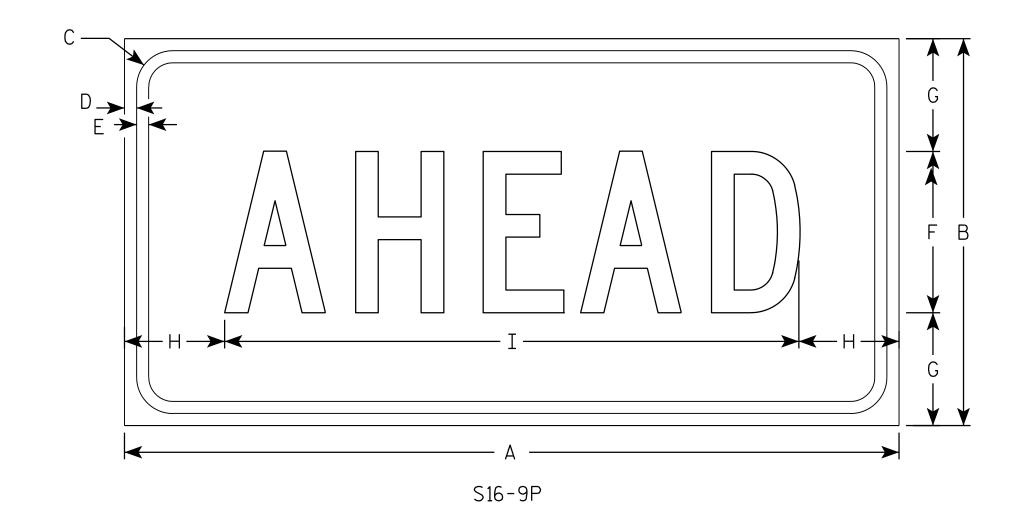
PROJECT NO:

PLOT DATE: 19-AUG-2013 14:55

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow-Green Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	U	٧	W	Х	Y	Z	Areo sq. ft.
1	24	12	1 1/8	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
25	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
2M	30	18	1 1/8	3⁄8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/8	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 ¾																		8.0
5																											

COUNTY:

STANDARD SIGN S16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch

For State Traffic Engineer

DATE 7/22/13

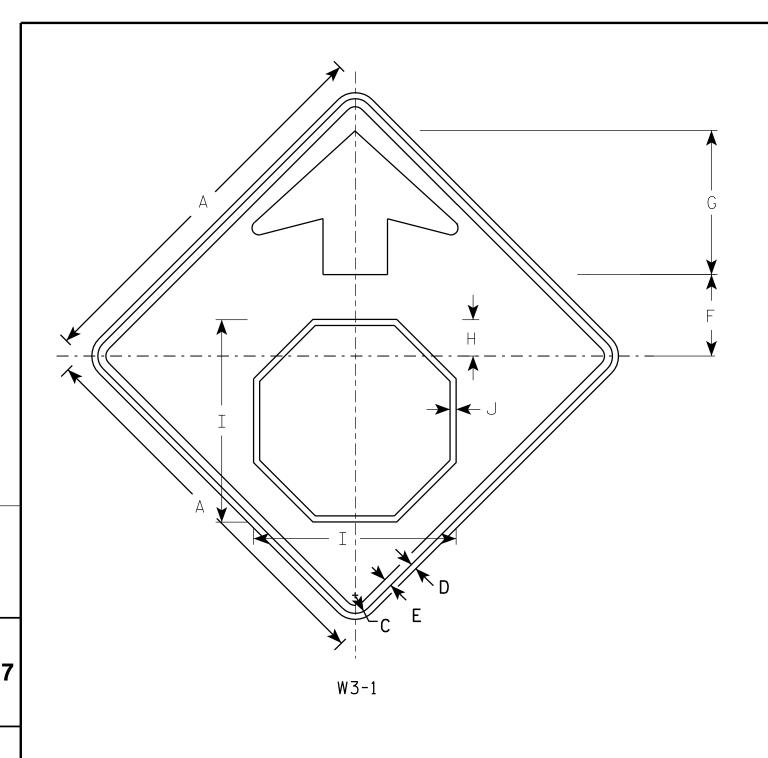
SHEET NO:

HWY:

PROJECT NO:

PLOT NAME :

PLATE NO. S16-9P.1

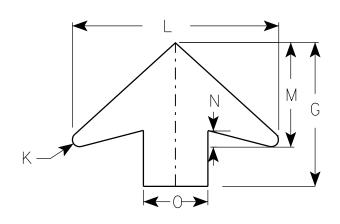


- 1. All Signs Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - YELLOW

Arrow & Border - BLACK

Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW	DFTAII
$\neg \cdots $	

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Ρ	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 1/8	15 ¾	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 %	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 %	6												9.0
2M	36		1 %	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 %	6												9.0
3	36		1 1/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 %	6												9.0
4	48		2 1/4	3/4	1	10	17 1/8	4 1/2	25 1/8	3/4	7 ⁄8	25 %	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 1/8	4 1/2	25 1/8	₹4	7 /8	25 %	13	2	8												16.0

STANDARD SIGN W3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew

For State Traffic Engineer

DATE 6/7/10 PLATE NO. W3-1.12

SHEET NO:

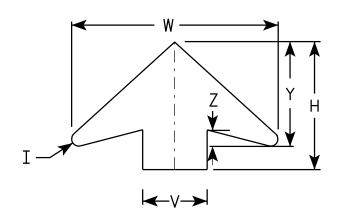
PROJECT NO:

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color: *

 Background YELLOW*

 Message BLACK
- 3. Message Series C for numbers Series E for wording
- 4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background



ARROW DETAIL

SIZE	Α	В	С	D	E	F	G	н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft
1																											
25	36		1 1/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 %	9.0
2M	36		1 %	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 %	9.0
3	36		1 %	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3∕8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 %	9.0
4	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	1 /8	30	2 1/4	4	1 1/4	15	10	1 %	1/2	8	9 1/4	9 3/8	12	8	25 %	3∕8	13	2	16.0
5	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7 ⁄8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0

STANDARD SIGN W3-5

WISCONSIN DEPT OF TRANSPORTATION

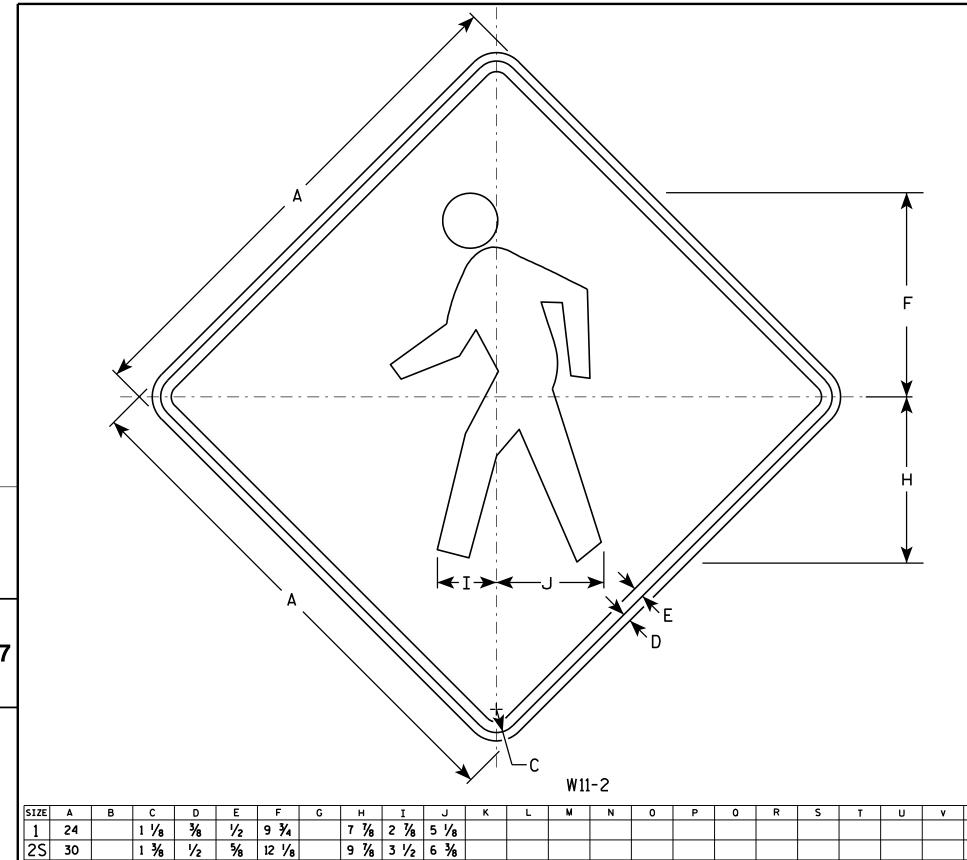
APPROVED

Matther R Rauch.

DATE 5/29/12 PLATE NO. W3-5.5

SHEET NO:

PROJECT NO:



<u>NOTES</u>

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

STANDARD SIGN W11-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE <u>6/7/10</u>

PLATE NO. W11-2.7

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W112.DGN

1 1/8

1 %

2 1/4 3/4

2M

3

4 48

5

PROJECT NO:

5/8

5/8

3/4

14 1/2

3/4 14 1/2

1 19 3/8

11 1/8 4 1/4 7 5/8

11 1/8 4 1/4 7 5/8

15 3/4 5 5/8 10 1/4

HWY:

PLOT DATE: 07-JUN-2010 13:29

COUNTY:

PLOT NAME :

PLOT BY: ditjph

4.0

6.25

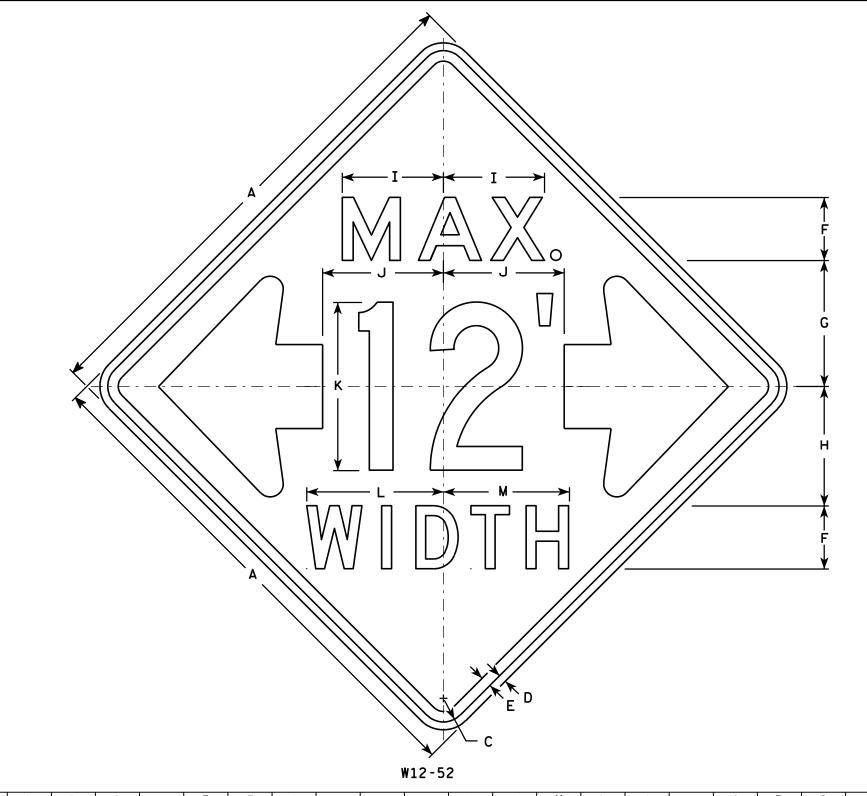
9.0

9.0

16.0

PLOT SCALE: 5.700818:1.000000

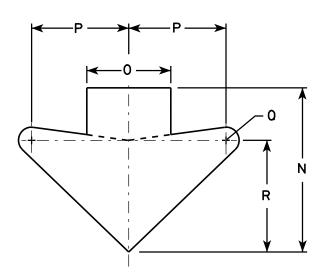
WISDOT/CADDS SHEET 42



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. The top line is series E, the numerals are series C, and the bottom line is series D.
- 6. Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

CT TE			T					ш			1/		1.4		_		_		_					· ·	·	7	Area
SIZE	Α	В	L	ט	-	F	G	Н	l I	J	K	L	M	N	U	P	U	R	>	1	U	V	W	X	T		Area sq. ft.
1																											
25	48		2 1/4	₹4	1	6	12	11 3/8	9 %	11 1/2	16	13	12	15 %	8	9 1/4	1 1/4	10 %									16.0
2M	48		2 1/4	₹4	1	6	12	11 3/8	9 %	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 %									16.0
3																											
4																											
5																											

COUNTY:

STANDARD SIGN W12-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

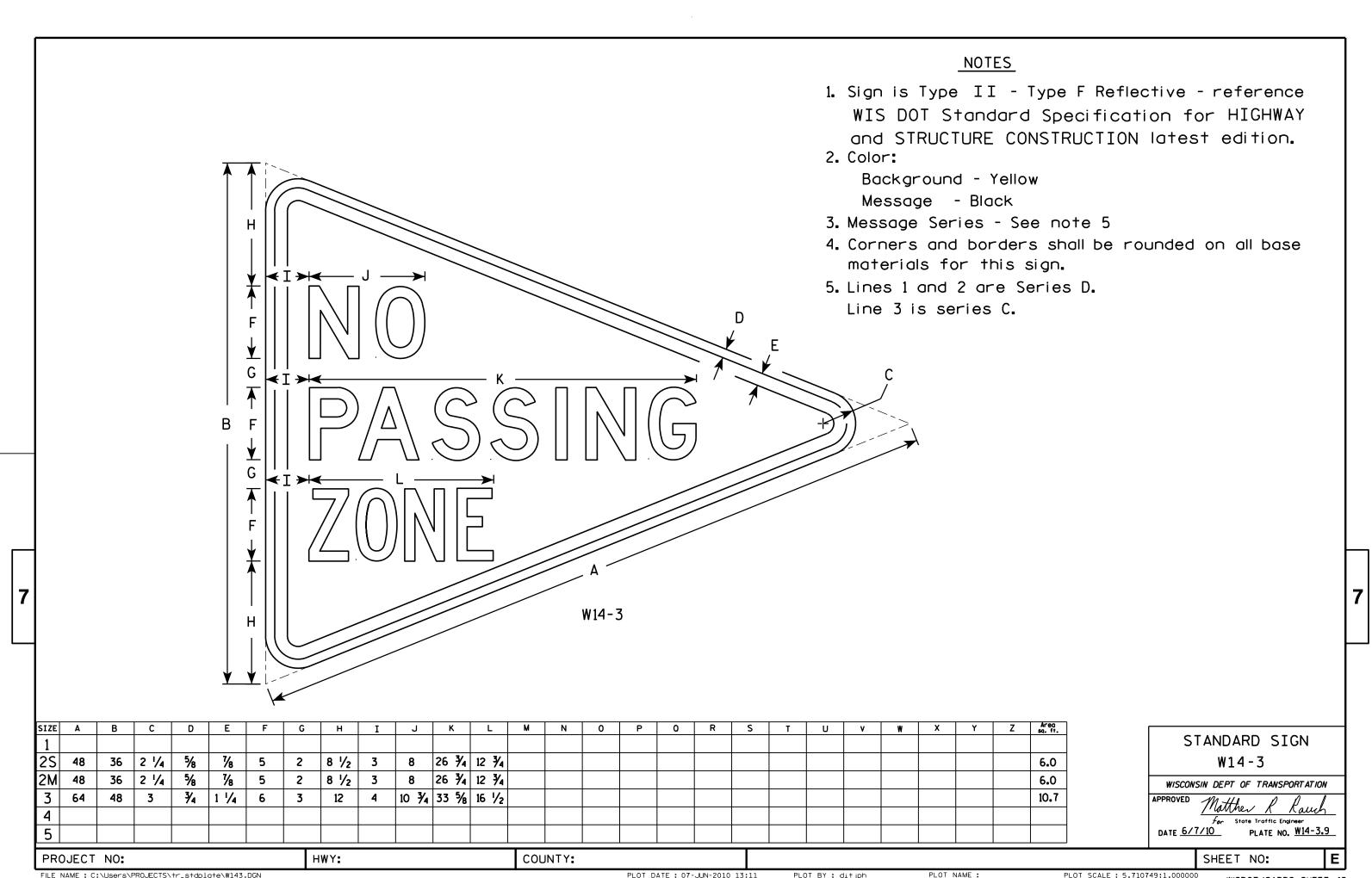
DATE 3/16/11 PLATE NO. W12-52.7

SHEET NO:

HWY:

PROJECT NO:

PLOT NAME :



FILE NAME : C:\Users\PROJECTS\tr_stdplate\W143.DGN

PLOT DATE: 07-JUN-2010 13:11

PLOT BY: ditjph

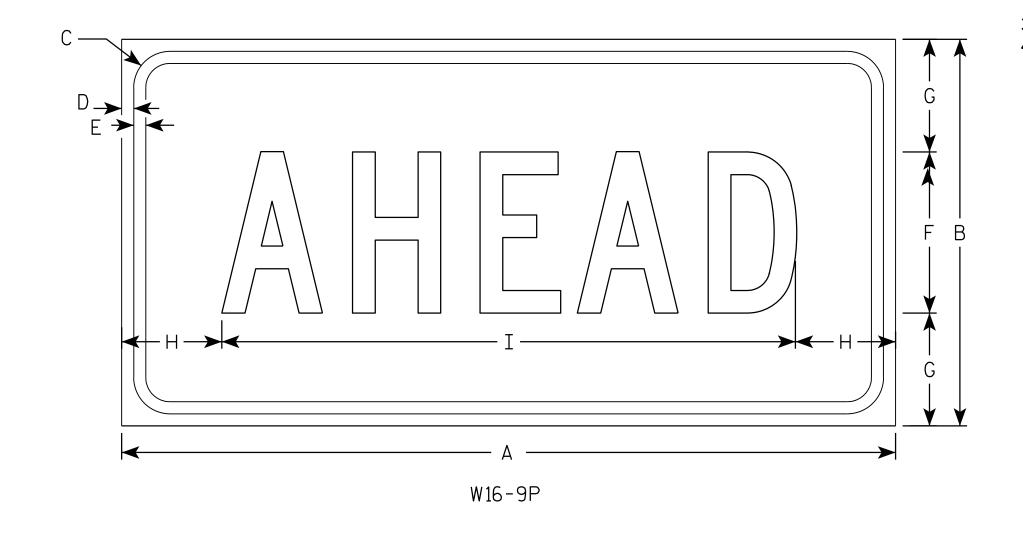
PLOT SCALE: 5.710749:1.000000

WISDOT/CADDS SHEET 42

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	Α	В	С	D	E	F	G	Η	I	J	K	L	M	N	0	P	0	R	S	T	U	٧	₩	Х	Y	Z	Areg sq. ft.
1																											
25	24	12	1 1/8	3/8	3⁄8	5	3 1/2	3 1/8	17 3/4																		2.0
2M	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/8	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 ¾																		8.0
5																											

COUNTY:

STANDARD SIGN W16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch
For State Traffic Engineer

DATE 12/28/10

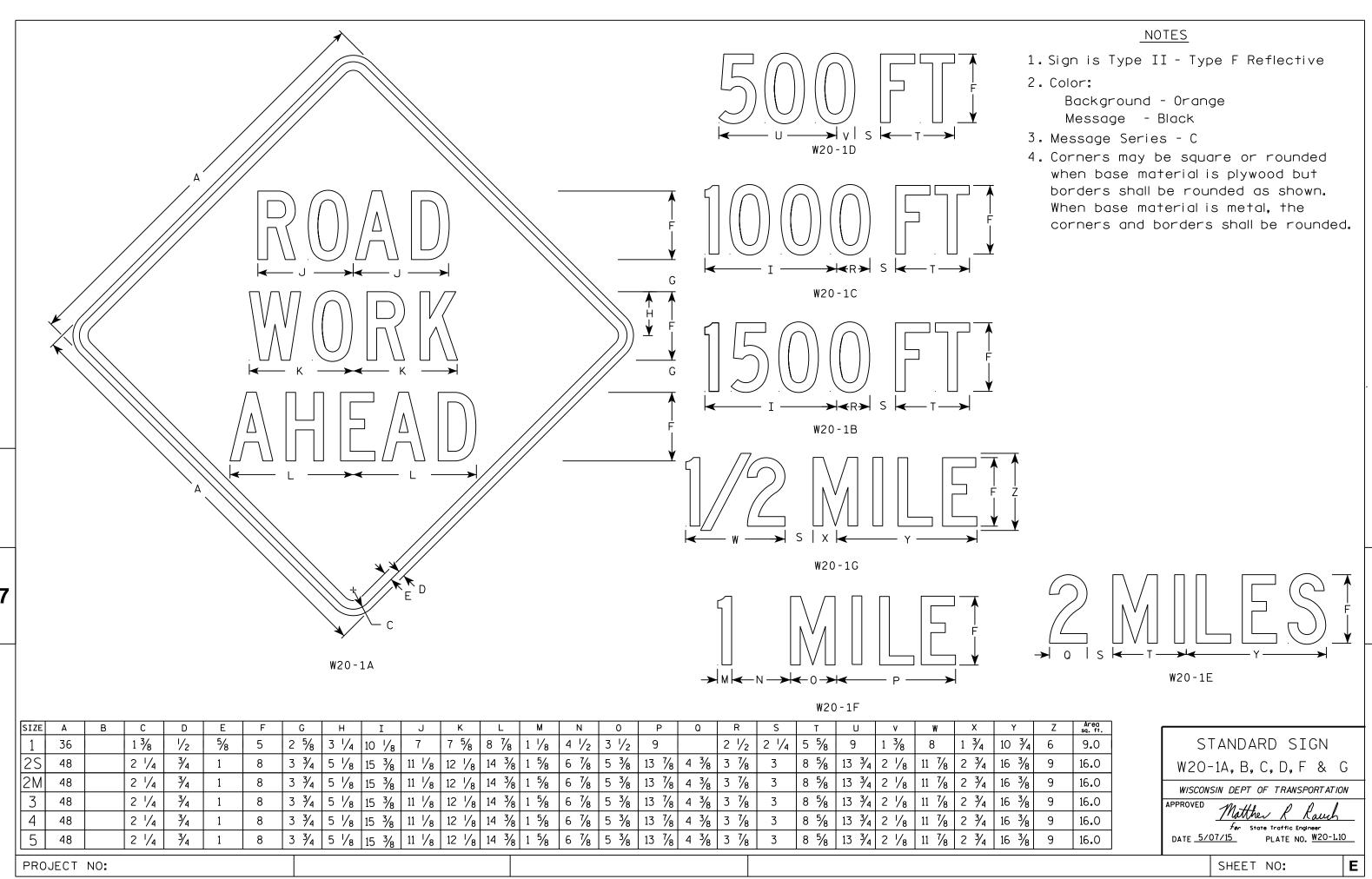
O PLATE NO. W16-9P.6

SHEET NO:

HWY:

PROJECT NO:

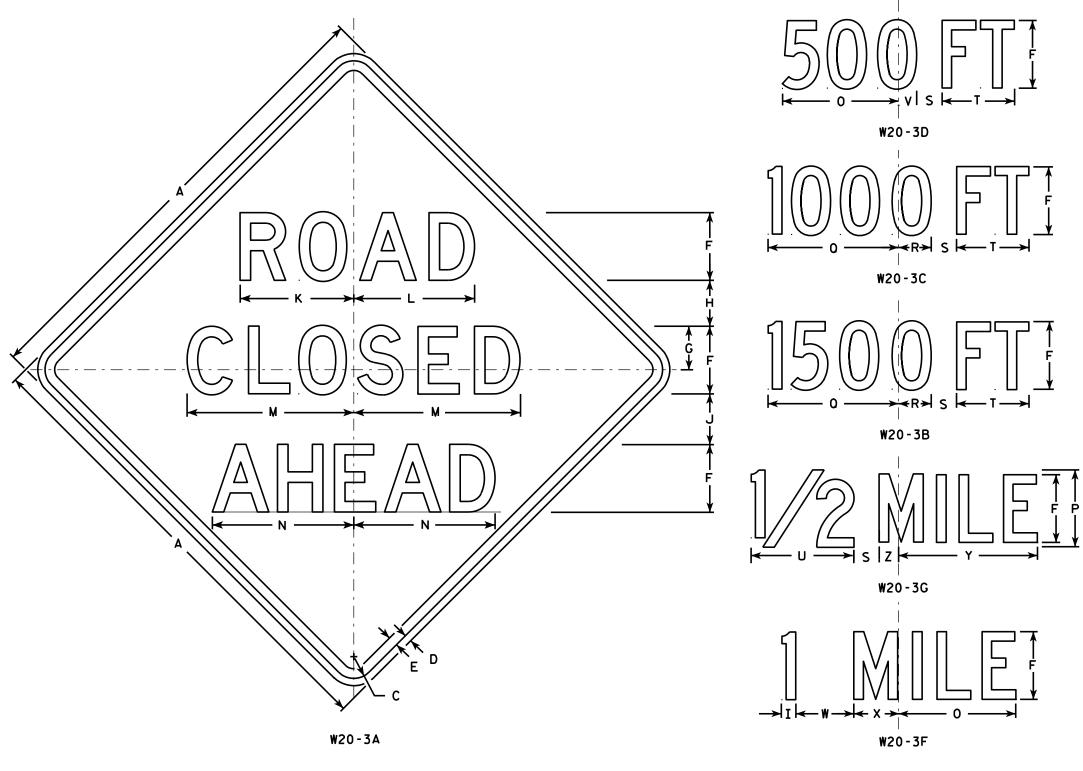
PLOT NAME :



FILE NAME . C.\CAFfiles\Projects\tr stdolote\W201 DCN

PLOT DATE . 01-DEC-2015 18.24

PIOT RY * \$\$ plotuser \$\$



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series see note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Lines 1 and 2 are Series D. Line 3 is Series D for AHEAD and Series C for all other distances.

1 % 5/8 ¾ 8 3/8 8 7/8 12 1/2 5 % 1 3/8 4 1/2 36 3 1/2 10 3/4 1 3/4 8 4 \(\frac{5}{8} \) 14 \(\frac{3}{8} \) 2 \(\frac{3}{8} \) 16.0 3/4 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 5/8 1 7/8 2M 3/4 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 48 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 % 1 % 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 3/4 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 4 % | 14 % | 2 % | 16.0 48 3/4 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 13 1/2 3 3/8 2 5/8 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 7 1/2 10 5/8 1 7/8 48 5 4 5/8 14 3/8 2 3/8 16.0 3/4 2 1/4 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 48

COUNTY:

STANDARD SIGN W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer DATE 3/18/11

PLATE NO. W20-3.7

SHEET NO:

PROJECT NO: FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN HWY:

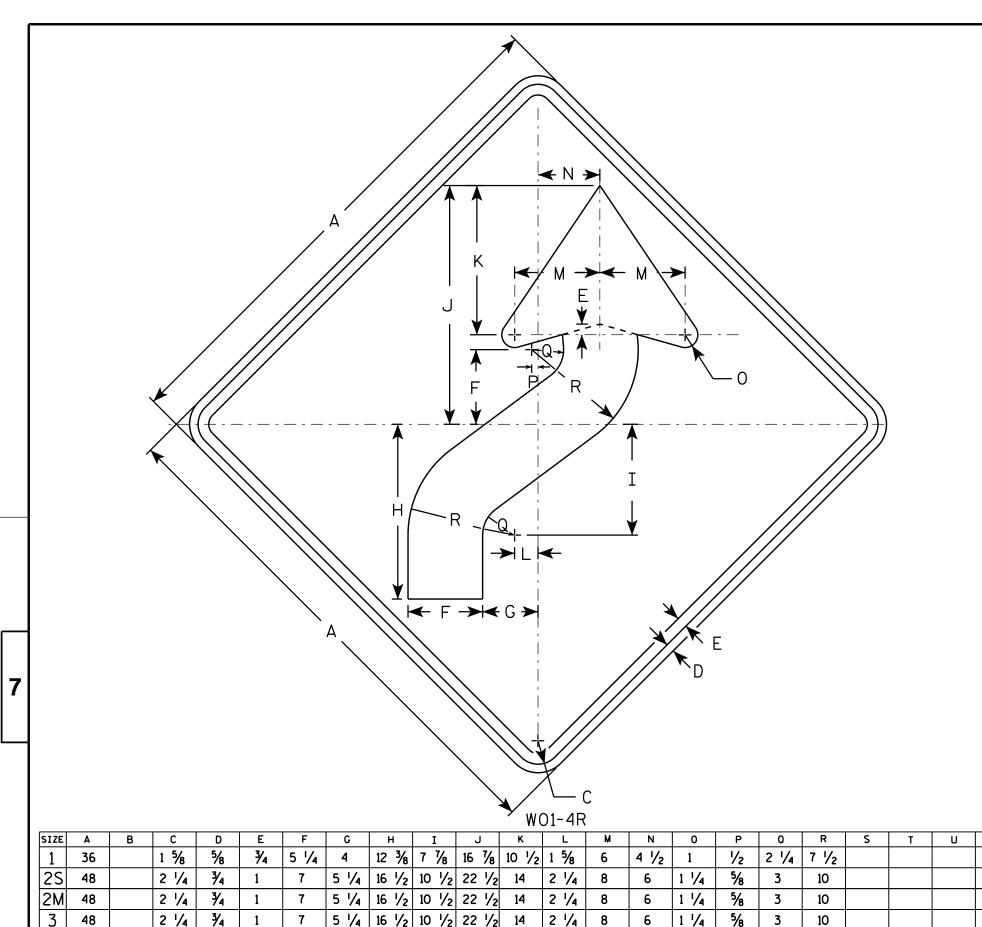
PLOT DATE: 18-MAR-2011 12:08

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: 9.931739:1.000000

WISDOT/CADDS SHEET 42



5 1/4 16 1/2 10 1/2 22 1/2 14

5 1/4 16 1/2 10 1/2 22 1/2 14

HWY:

2 1/4

2 1/4

NOTES

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

9.0 16.0 16.0 16.0 16.0 STANDARD SIGN W01-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch

For State Traffic Engineer

DATE <u>11/18/1</u>3

PLATE NO. WO1-4.1
SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W014.DGN

48

48

PROJECT NO:

2 1/4 3/4

2 1/4 | 3/4

PLOT DATE : 28-FEB-2014 11:35

10

1 1/4

1 1/4

COUNTY:

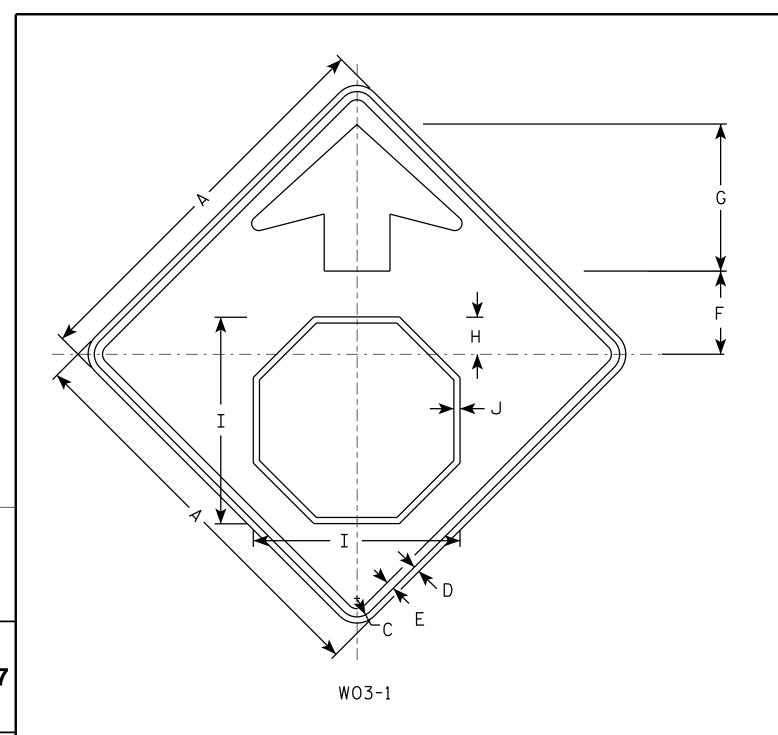
5/8

PLOT NAME :

PLOT BY: mscj9h

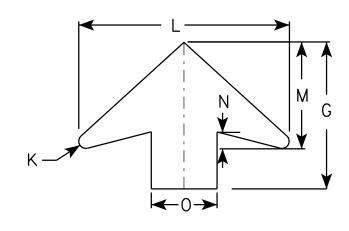
PLOT SCALE: 6.755110:1.000000

WISDOT/CADDS SHEET 42



- 1. All Signs Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - ORANGE Arrow & Border - BLACK Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

SIZE	Α	В	С	D	Е	F	G	Н	I	C	K	L	М	N	0	Р	0	R	S	Т	С	٧	W	X	Υ	Z	Areo sq. ft.
1	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 %	6												9.0
2S	48		2 1/4	3/4	1	10	17 1/8	4 1/2	25 1/8	3/4	7∕8	25 %	13	2	8												16.0
2M	48		2 1/4	3/4	1	10	17 1/8	4 1/2	25 1/8	3∕4	7∕8	25 %	13	2	8												16.0
3	48		2 1/4	3∕4	1	10	17 1/8	4 1/2	25 1/8	3∕4	7∕8	25 %	13	2	8												16.0
4	48		2 1/4	3∕4	1	10	17 1/8	4 1/2	25 1/8	3∕4	7∕8	25 %	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 1/8	4 1/2	25 1/8	3/4	7 ⁄8	25 %	13	2	8						·						16.0

STANDARD SIGN WO3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVE

For sure of the

State Traffic Engine

DATE 11/20/13 PLATE NO. W03-1.1

SHEET NO:

PROJECT NO:

FILE NAME: C:\CAEFiles\Projects\tr_stdplote\W031.DGN

PLOT DATE: 20-NOV-2013 10:54

PLOT BY: ms

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

W057-52

* See note 5

SIZE	Α	В	С		D	Е	F	G	Н	I	J	К	L	M	N	0	ρ	0	R	S	T	U	v	W	Х	Y	Z	Area sq. ft.
1	36	24	1 1/	3	3/8	1/2	6	4 1/2	3	4 3/4	14 %	10 %	11 3/8	2	13													6.0
2S	48	36	1 3/	3	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0
2M	48	36	1 3/	3	1/2	5/8	8	7	6	6 %	19 1/2	14	15	2 3/4	17 3/8													12.0
3	48	36	1 3/	3	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0
4	48	36	1 3/	3	1/2	5/8	8	7	6	6 %	19 1/2	14	15	2 3/4	17 3/8													12.0
5	48	36	1 3/	3	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0

COUNTY:

STANDARD SIGN W057-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 11/20/13

SHEET NO:

HWY:

PROJECT NO:

PLOT BY: mscj9h

PLATE NO. W057-52.1

STH 33 S	TAGE 1		AREA (S	iF)	INCREMENTA	AL VOL (CY) (I	UNADJUSTED)	CUMULAT	IVE VOL (CY)	
STATION	DISTANCE	CUT	FILL	EBS	CUT NOTE 1	FILL NOTE 3	EBS	CUT 1.00 NOTE 1	EXPANDED FILL 1.25	MASS ORDINATE NOTE 4
60+35.40	_	3.5	0.0	0.2	-	_	_	0.0	0.0	0
60+50.00	15	3.5	0.0	0.2	1.9	0.0	0.1	1.9	0.0	2
60+61.06	11	3.6	0.0	0.2	1.4	0.0	0.1	3.3	0.0	3
61+00.00	39	3.6	1.5	0.2	5.2	1.1	0.3	8.5	1.4	7
61+50.00	50	3.5	5.4	0.2	6.6	6.4	0.3	15. 1	9.4	6
61+86.96	37	3.4	7.5	0.2	4.7	8.9	0.2	19.8	20.5	-1
62+00.00	13	4.0	2.7	0.2	1.8	2.5	0.1	21.6	23.6	-2
62+50.00	50	4.2	2.0	0.2	7.5	4.3	0.4	29.1	28.9	0
63+00.00	50	4.7	0.5	0.2	8.2	2.3	0.4	37.4	31.8	6
63+50.00	50	4.1	0.6	0.2	8.2	1.0	0.4	45.5	33.0	12
64+00.00	50	4.7	0.2	0.2	8.1	0.7	0.4	53.7	33.9	20
64+15.58	16	5.3	0.0	0.3	2.9	0.0	0.1	56.6	33.9	23
64+50.00	34	6.8	0.0	0.3	7.7	0.0	0.4	64.3	33.9	30
64+80.00	30	3.6	0.4	0.2	5.7	0.2	0.3	70.0	34.2	36
65+00.00	20	7.7	0.0	0.4	4.2	0.2	0.2	74.2	34.4	40
65+50.00	50	6.6	0.0	0.3	13.2	0.0	0.7	87.4	34.4	53
66+00.00	50	3.9	0.0	0.2	9.7	0.0	0.5	97.1	34.4	63
66+44.20	44	3.8	0.0	0.2	6.3	0.0	0.3	103.4	34.4	69
66+50.00	6	3.9	0.0	0.2	0.8	0.0	0.0	104.3	34.4	70
67+00.00	50	5.4	0.0	0.3	8.6	0.0	0.4	112.8	34.4	78
67+15.00	15	1.7	0.6	0.1	2.0	0.2	0.1	114.8	34.6	80
67+50.00	35	3.4	0.0	0.2	3.2	0.4	0.2	118.0	35.1	83
68+00.00	50	4.4	0.0	0.2	7.2	0.0	0.4	125.2	35.1	90
68+30.00	30	1.5	0.1	0.1	3.3	0.0	0.2	128.5	35.2	93
68+50.00	20	5.3	0.0	0.3	2.5	0.0	0.1	131.0	35.2	96
69+00.00	50	4.9	0.0	0.2	9.4	0.0	0.5	140.4	35.2	105
69+50.00	50	3.9	2.5	0.2	8.1	2.3	0.4	148.5	38.0	110
70+00.00	50	3.9	1.7	0.2	7.2	3.8	0.4	155.7	42.8	113
70+50.00	50	4.9	0.0	0.2	8.1	1.6	0.4	163.8	44.8	119
70+75.00	25	1.8	0.0	0.1	3.1	0.0	0.2	166.9	44.8	122
71+00.00	25	5.3	0.0	0.3	3.3	0.0	0.2	170.2	44.8	125
71+50.00	50	1.5	1.9	0.1	6.3	1.8	0.3	176.4	47.0	129
72+00.00	50	3.9	1.1	0.2	5.0	2.8	0.3	181.4	50.5	131
72+50.00	50	5.3	0.5	0.3	8.5	1.5	0.4	189.9	52.3	138
73+00.00	50	3.8	0.0	0.2	8.4	0.4	0.4	198.3	52.9	145
73+50.00	50	3.8	0.0	0.2	7.1	0.0	0.4	205.4	52.9	152
74+00.00	50	5.5	0.1	0.3	8.7	0.1	0.4	214.1	53.0	161
74+28.56	29	5.4	0.0	0.3	5.8	0.1	0.3	219.8	53.0	167
74+63.15		5,3	0.1	0.3				219.8	53.0	167
75+00.00	37	6.0	0.0	0.3	7.7	0.0	0.4	227.5	53.1	174

- 1) CUT: CUT INCLUDES UNUSABLE PAVEMENT MATERIAL
- 2) FILL: FILL DOES NOT INCLUDE UNUSABLE PAVEMENT MATERIAL
- 3) MASS ORDINATE: MASS ORDINATE = (CUT UNUSABLE PAVEMENT MATERIAL) (FILL * FILL FACTOR)

CON'T NEXT SHEET

9

SHEET PROJECT NO:5100-08-72 HWY:STH 33 COUNTY: MONROE Ε EARTHWORK

PLOT BY: JACK, ROBERT A. PLOT NAME:

STH 33 S	TAGE 1		AREA (S	F)	INCREMENTA	AL VOL (CY) (I	UNADJUSTED)	CUMULAT	IVE VOL (CY)	
STATION	DISTANCE	CUT	EUI	EDG	CUT NOTE 1	FILL NOTE 3	EDG	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
75+50.00	DISTANCE	CUT 5.6	FILL 0.1	EBS 0.3	10.7	0.1	EB\$ 0.5	NOTE 1 238.2	53.2	NOTE 4 185
75+80.00 75+80.00	50 30	3.5	0.1	0.3	5.1	0.1	0.3	243.3	53.3	190
76+00.00		5.3	0.1	0.3	3.3	0.1	0.3	243.3 246.6	53.4	193
76+50.00 76+50.00	20 50	5.3 4.3	0.0	0.3	3.3 8.9	0.1	0.4	246.6 255.4	53.4	202
76±50.00 77±00.00	50	4.3 5.9	0.0	0.3	9.5	0.1	0.4	264.9	53.4	202 211
77+50.00	50	5.8	0.0	0.3	10.9	0.0	0.5	275.8	53.4	222
77±50.00 78±00.00		5.6 5.1		0.3	10.9	0.0	0.5	275.6 285.9		
	50 50	5. l 4.4	0.2 0.3		8.8		0.4		53.6 54.1	232
78+50.00	50	4.4 3.6	0.3	0.2	0.6 1.5	0.4 0.0	0.4	294.6	54.1	241 242
78+60.00	10			0.2			I I	296.1		
78+80.00	20	3.4	0.2	0.2	2.6	0.1	0.1	298.7	54.3	244
79+00.00	20	5.1 4.6	0.1 0.2	0.3	3.1 9.0	0.1 0.3	0.2 0.4	301.8	54.4 54.7	247
79+50.00 79+79.26	50	4.6 3.5	0.2	0.2 0.2	9.0 4.4	0.3	0.4	310.8 315.2	54.7 54.9	256 260
80+00.00	29 21	3.2	0.1	0.2	4. 4 2.6	0.2	0.2	315.2	55.0	263
80+50.00	50	5.5	0.1	0.3	8.1	0.1	0.4	317.6 325.8	55.0 55.2	263 271
80+52.48	2	5.3	0.1	0.3	0.5	0.2	0.4	326.3	55.3	271
80+98.60	2	5.3 4.8	0.5	0.2	0.5	0.0	0.0	326.3	55.3	271
81+00.00	1 1	4.9	0.5	0.2	0.3	0.0	0.0	326.6	55.3	271
81+50.00	50	5.4	0.0	0.3	9.5	0.0	0.5	336.1	55.8	280
82+00.00	50	5.4 5.5	0.0	0.3	10.1	0.4	0.5	346.2	56.0	290
82+07.09	7	5.0	0.6	0.3	1.4	0.1	0.1	347.6	56.1	292
82+54.23	,	4.5	0.8	0.2	1.4	"	0.1	347.6	56.1	292
83+00.00	46	5.4	0.0	0.3	8.4	0.7	0.4	356.1	57.0	299
83+25.00	25	4.1	0.0	0.2	4.4	0.0	0.2	360.4	57.0	303
83+50.00	25	5.8	0.0	0.3	4.6	0.0	0.2	365.0	57.0	308
84+00.00	50	5.7	0.0	0.3	10.6	0.0	0.5	375.6	57.0	319
84+15.00	15	4.2	0.0	0.2	2.8	0.0	0.1	378.4	57.0	321
84+50.00	35	5.3	0.3	0.3	6.2	0.2	0.3	384.5	57.3	327
84+60.00	10	4.0	0.0	0.2	1.7	0.1	0.1	386.2	57.4	329
85+00.00	40	5.5	0.3	0.3	7.1	0.2	0.4	393.3	57.7	336
85+30.00	30	4.1	0.0	0.2	5.4	0.2	0.3	398.7	57.9	341
85+50.00	20	5.8	0.3	0.3	3.7	0.1	0.2	402.3	58.0	344
85+65.00	15	4.3	0.0	0.2	2.8	0.1	0.1	405.1	58.1	347
86+00.00	35	5.7	0.7	0.3	6.5	0.4	0.3	411.6	58.7	353
86+50.00	50	3.6	0.2	0.2	8.6	0.7	0.4	420.3	59.6	361
87+00.00	50	5.5	0.6	0.3	8.4	0.7	0.4	428.7	60.5	368
87+33.26	33	5.5	0.8	0.3	6.8	0.9	0.3	435.5	61.6	374
87+50.00	17	5.6	0.6	0.3	3.4	0.4	0.2	438.9	62.2	377
87+62.91	13	4.3	0.8	0.2	2.4	0.3	0.1	441.3	62.6	379
88+11.53		6.1	0.6	0.3				441.3	62.6	379

NOTES:

- 1) CUT: CUT INCLUDES UNUSABLE PAVEMENT MATERIAL
- 2) FILL: FILL DOES NOT INCLUDE UNUSABLE PAVEMENT MATERIAL
- 3) MASS ORDINATE: MASS ORDINATE = (CUT UNUSABLE PAVEMENT MATERIAL) (FILL * FILL FACTOR)

CON'T NEXT SHEET

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET E

9

9

STH 33 S	TAGE 1		AREA (S	F)	INCREMENTA	AL VOL (CY) (I	UNADJUSTED)	CUMULAT	IVE VOL (CY)	
STATION	DISTANCE	CUT	FILL	EBS	CUT NOTE 1	FILL NOTE 3	EBS	CUT 1.00 NOTE 1	EXPANDED FILL 1.25	MASS ORDINATE NOTE 4
88+50.00	38	7.3	0.0	0.4	9.5	0.4	0.5	450.8	63.1	388
88+78.11	28	6.8	0.0	0.3	7.3	0.0	0.4	458.1	63.1	395
89+00.00	22	6.7	0.0	0.3	5.5	0.0	0.3	463.6	63.1	400
89+50.00	50	7.7	0.0	0.4	13.3	0.0	0.7	476.9	63.1	414
90+00.00	50	6.6	0.0	0.3	13.3	0.0	0.7	490.2	63.1	427
90+22.95	23	6.4	0.0	0.3	5.5	0.0	0.3	495.7	63.1	433
90+35.00	12	4.6	0.0	0.2	2.5	0.0	0.1	498.2	63.1	435
90+50.00	15	6.4	0.0	0.3	3.1	0.0	0.2	501.2	63.1	438
91+00.00	50	6.7	0.0	0.3	12.1	0.0	0.6	513.4	63.1	450
91+50.00	50	6.6	0.1	0.3	12.3	0.1	0.6	525.7	63.2	462
91+56.30	6	6.6	0.1	0.3	1.5	0.0	0.1	527.2	63.3	464
92+00.00	44	7.0	0.0	0.4	11.1	0.1	0.6	538.3	63.4	475
92+50.00	50	5.8	0.8	0.3	11.9	0.7	0.6	550.1	64.3	486
93+00.00	50	5.9	0.4	0.3	10.8	1.1	0.5	560.9	65.7	495
93+07.72	8	6.0	0.4	0.3	1.7	0.1	0.1	562.6	65.9	497
93+50.00	42	8.3	0.1	0.4	11.2	0.4	0.6	573.8	66.3	507
93+70.00	20	6.7	0.0	0.3	5.5	0.0	0.3	579.4	66.4	513
93+94.52	25	8.2	0.5	0.4	6.8	0.2	0.3	586.1	66.7	519
94+00.00	5	8.5	0.4	0.4	1.7	0.1	0.1	587.8	66.8	521
94+50.00	50	5.4	0.2	0.3	12.9	0.5	0.6	600.7	67.4	533
94+60.00	10	5.3	0.1	0.3	2.0	0.0	0.1	602.7	67.5	535
94+81.32	21	6.7	0.4	0.3	4.7	0.2	0.2	607.4	67.7	540
95+00.00	19	6.5	0.4	0.3	4.6	0.3	0.2	611.9	68.1	544
95+50.00	50	6.1	0.7	0.3	11.7	1.0	0.6	623.6	69.4	554
95+75.00	25	4.7	0.0	0.2	5.0	0.3	0.2	628.6	69.8	559
96+00.00	25	6.3	0.3	0.3	5.1	0.1	0.3	633.7	69.9	564
96+50.00	50	7.1	0.1	0.4	12.4	0.3	0.6	646.1	70.3	576
96+54.68	5	6.9	0.1	0.3	1.2	0.0	0.1	647.3	70.3	577
96+80.00	25	5.0	0.0	0.2	5.5	0.1	0.3	652.8	70.4	582
97+00.00	20	6.6	0.4	0.3	4.3	0.1	0.2	657.1	70.6	587
97+50.00	50	7.5	0.1	0.4	13.1	0.4	0.7	670.2	71.0	599
98+00.00	50	7.8	0.0	0.4	14.2	0.1	0.7	684.4	71.1	613
98+50.00	50	5.0	0.0	0.3	11.9	0.0	0.6	696.2	71.2	625
99+00.00	50	6.5	0.2	0.3	10.6	0.1	0.5	706.8	71.4	635
99+50.00	50	23.0	0.0	1.1	27.3	0.1	1.4	734.1	71.5	663
99+64.94	15	42.4	8.0	2.1	18.1	0.2	0.9	752.2	71.8	680
100+26.17		55.1	0.3	2.8				752.2	71.8	680
100+50.00	24	21.4	0.1	1.1	33.8	0.2	1.7	785.9	72.0	714
101+00.00	50	5.9	0.0	0.3	25.3	0.0	1.3	811.2	72.1	739
101+50.00	50	6.8	0.0	0.3	11.8	0.0	0.6	823.0	72.1	751

NOTES:

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- 2) FILL: FILL DOES NOT INCLUDE UNUSABLE PAVEMENT MATERIAL
- 3) MASS ORDINATE: MASS ORDINATE = (CUT UNUSABLE PAVEMENT MATERIAL) (FILL * FILL FACTOR)

CON'T NEXT SHEET

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET E

PLOT SCALE : 1 IN:10 FT

STH 33 S	TAGE 1		AREA (S	F)	INCREMENTA	AL VOL (CY) (I	UNADJUSTED)	CUMULAT	IVE VOL (CY)	
STATION	DISTANCE	CUT	FILL	EB\$	CUT NOTE 1	FILL NOTE 3	EBS	CUT 1.00 NOTE 1	EXPANDED FILL 1.25	MASS ORDINATE NOTE 4
102+00.00	50	7.1	0.0	0.4	12.8	0.0	0.6	835.8	72.1	764
102+10.00	10	6.6	0.0	0.3	2.5	0.0	0.1	838.4	72.1	766
102+50.00	40	7.1	0.0	0.4	10.2	0.0	0.5	848.6	72.1	776
102+75.00	25	6.7	0.0	0.3	6.4	0.0	0.3	855.0	72.2	783
103+00.00	25	7.3	0.0	0.4	6.5	0.0	0.3	861.4	72.2	789
103+50.00	50	7.2	0.0	0.4	13.4	0.0	0.7	874.8	72.2	803
104+00.00	50	6.4	0.0	0.3	12.6	0.0	0.6	887.4	72.2	815
104+50.00	50	7.2	0.0	0.4	12.6	0.0	0.6	900.0	72.2	828
104+70.00	20	6.4	0.0	0.3	5.0	0.0	0.3	905.0	72.2	833
105+00.00	30	7.1	0.0	0.4	7.5	0.0	0.4	912.5	72.2	840
105+25.00	25	6.3	0.0	0.3	6.2	0.0	0.3	918.7	72.2	846
105+50.00	25	7.2	0.0	0.4	6.3	0.0	0.3	925.0	72.2	853
105+71.12	21	6.9	0.0	0.3	5.5	0.0	0.3	930.5	72.3	858
106+00.00	29	8.2	0.0	0.4	8.1	0.0	0.4	938.6	72.3	866
106+20.00	20	8.2	0.0	0.4	6.1	0.0	0.3	944.7	72.3	872
106+45.00	25	7.9	0.0	0.4	7.5	0.0	0.4	952.1	72.3	880
106+50.00	5	8.4	0.0	0.4	1.5	0.0	0.1	953.6	72.3	881
106+63.47	13	7.2	0.0	0.4	3.9	0.0	0.2	957.5	72.3	885
107+00.00	37	4.6	0.0	0.2	8.0	0.0	0.4	965.5	72.3	893
107+50.00	50	7.7	0.0	0.4	11,4	0.0	0.6	976.9	72.4	905
107+55.83	6	7.9	0.0	0.4	1.7	0.0	0.1	978.6	72.4	906
107+65.00	9	8.0	0.0	0.4	2.7	0.0	0.1	981.3	72.4	909
108+00.00	35	9.5	0.0	0.5	11.3	0.0	0.6	992.6	72.4	920
108+25.00	25	8.4	0.0	0.4	8.3	0.0	0.4	1,000.9	72.4	929
108+50.00	25	9.5	0.0	0.5	8.3	0.0	0.4	1,009.2	72.4	937
109+00.00	50	9.4	0.0	0.5	17.5	0.0	0.9	1,026.7	72.4	954
109+42.17	42	8.6	0.0	0.4	14.0	0.0	0.7	1,040.7	72.4	968
109+50.00	8	8.4	0.1	0.4	2.5	0.0	0.1	1,043.2	72.4	971
109+56.47	6	8.3	0.1	0.4	2.0	0.0	0.1	1,045.2	72.4	973
109+70.77	14	7.7	0.0	0.4	4.3	0.0	0.2	1,049.4	72.5	977
109+80.00	9	7.8	0.0	0.4	2.7	0.0	0.1	1,052.1	72.5	980
110+00.00	20	9.5	0.0	0.5	6.4	0.0	0.3	1,058.5	72.5	986
110+50.00	50	10.8	0.0	0.5	18.8	0.0	0.9	1,077.3	72.5	1,005
110+54.21	4	10.6	0.0	0.5	1.7	0.0	0.1	1,079.0	72.5	1,007
111+00.00	46	7.6	0.0	0.4	15.4	0.0	0.8	1,094.4	72.5	1,022
111+50.00	50	8.4	0.0	0.4	14.9	0.0	0.7	1,109.3	72.5	1,037
111+64.75	15	7.7	0.0	0.4	4.4	0.0	0.2	1,113.7	72.5	1,041
111+70.33	6	6.4	0.5	0.3	1.5	0.0	0.1	1,115.2	72.5	1,043
112+08.99	45	6.9	0.4	0.3	2.0	0.4	00	1,115.2	72.5	1,043
112+21.75	13	9.3	0.0	0.5	3.8	0.1	0.2	1,119.0	72.6	1,046

NOTES:

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CON'T NEXT SHEET

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET **E**

9

PLOT SCALE : 1 IN:10 FT

9

STH 33 S	TAGE 1		AREA (S	F)	INCREMENTA	AL VOL (CY) (UNADJUSTED)	CUMULAT	IVE VOL (CY)	
STATION	DISTANCE	CUT	FILL	EBS	CUT NOTE 1	FILL NOTE 3	EBS	CUT 1.00 NOTE 1	EXPANDED FILL 1.25	MASS ORDINATE NOTE 4
112+50.00	28	9.7	0.0	0.5	9.9	0.0	0.5	1,128.9	72.6	1,056
113+00.00	50	9.8	0.0	0.5	18.0	0.0	0.9	1,146.9	72.6	1,074
113+50.00	50	9.3	0.1	0.5	17.6	0.1	0.9	1,164.5	72.7	1,092
114+00.00	50	8.0	0.0	0.4	15.9	0.1	0.8	1,180.5	72.8	1,108
114+50.00	50	10.0	0.0	0.5	16.6	0.0	0.8	1,197.1	72.8	1,124
115+00.00	50	9.9	0.0	0.5	18.4	0.0	0.9	1,215.5	72.8	1,143
115+35.66	36	10.0	0.0	0.5	13.1	0.0	0.7	1,228.6	72.8	1,156
115+42.84	7	8.9	0.1	0.4	2.5	0.0	0.1	1,231.1	72.8	1,158
			COLU	MN TOTALS	1,231.1	58.3	61.6			

NOTES:

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STH 33		AREA (SF)			INCREMEN	ITAL VOL (CY) (UN	IADJUSTED)	CUMULATIVE VOL (CY)		
STATION	DISTANCE	CUT	FILL	EBS	CUT NOTE 1	FILL NOTE 3	EBS	CUT 1.00 NOTE 1	EXPANDED FILL 1.25	MASS ORDINATE NOTE 4
60+35.40		5.2	0.0	0.3				0.0	0.0	0
60+50.00	15	5.2	0.0	0.3	2.8	0.0	0.1	2.8	0.0	3
61+00.00	50	9.1	0.0	0.5	13.3	0.0	0.7	16.1	0.0	16
61+50.00	50	20.9	0.0	1.0	27.8	0.0	1.4	43.9	0.0	44
62+00.00	50	135.4	0.0	6.8	144.7	0.0	7.2	188.6	0.0	189
62+50.00	50	112.1	4.5	5.6	229.2	4.2	11.5	417.8	5.2	413
63+00.00	50	86.8	11.3	4.3	184.2	14.7	9.2	601.9	23.6	578
63+50.00	50	69.6	17.1	3.5	144.8	26.3	7.2	746.8	56.5	690
64+00.00	50	60.1	17.9	3.0	120.1	32.4	6.0	866.9	97.0	770
64+50.00	50	64.8	13.5	3.2	115.6	29.1	5.8	982.5	133.3	849
64+75.00	25	56.8	10.5	2.8	56.3	11.1	2.8	1,038.8	147.2	892
65+00.00	25	71.9	5.8	3.6	59.6	7.6	3.0	1,098.5	156.7	942
65+50.00	50	80.6	0.5	4.0	141.2	5.8	7.1	1,239.7	164.0	1,076
66+00.00	50	101.3	0.0	5.1	168.4	0.5	8.4	1,408.1	164.5	1,244
66+50.00	50	129.1	15.9	6.5	213.3	14.7	10.7	1,621.4	183.0	1,438

NOTES:

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CON'T NEXT SHEET

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET E

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STH 33	AREA (SF)			INCREMEN	TAL VOL (CY) (UN	ADJUSTED)	CUI			
STATION	DISTANCE	CUT	2 11.1	EDG	CUT NOTE 1	FILL NOTE 3	EDC	CUT 1.00 NOTE 1	EXPANDED FILL	MASS ORDINATE
STATION 67+00.00	50	CUT 156.0	FILL 2.1	EBS 7.8	264.0		EBS		1. 25 203.7	NOTE 4
I I		I I				16.6	13.2	1,885.4		1,682
67+15.00 67+42.19	15 27	162.2 162.8	1. 4 1.6	8.1 8.1	88.4 163.6	0.9 1.5	4.4	1,973.7	204.9 206.8	1,769
67+42.19	2 <i>1</i> 8	158.6	2.0	7.9	46.5	0.5	8.2 2.3	2,137.3 2,183.8	207.4	1,931 1,976
68+00.00	50	143.9	2.8	7.2	46.5 280.1		14.0		213.0	
68+25.00	25	138.7	3.8	6.9	130.8	4.4 3.1	6.5	2,463.9 2,594.8	216.8	2,251 2,378
68+50.00	25 25	134.3	3.6 4.6	6.7	126.4	3.1	6.3	2,721.2	221.7	
69+00.00	50	120.2	7.4	6.0	235.6	11.1	11.8	2,721.2	235.6	2,499 3,731
69+50.00	50 50	111.3	7. 4 8.1	5.6	214.4		10.7	3,171.2	253.5	2,721
		I I				14.4				2,918
70+00.00	50 50	103.0 97.4	12.7 16.7	5.1 4.9	198.4 185.5	19.3 27.2	9.9	3,369.6 3,555.1	277.7 311.7	3,092
70+50.00 70+75.00	25	97.4 96.8	16.3	4.8	89.9	15.3	9.3 4.5	3,645.0	330.8	3,243 3,314
71+00.00	25 25	96.6 97.6	16.3		90.0	15.1	4.5	3,734.9	349.7	
71+50.00	25 50	106.6	14.9	4.9 5.3	189.0	28.8	9.5	3,734.9	385.7	3,385 3,538
72+00.00	50 50	115.6	13.0	5.8 5.8	205.7	25.7	10.3	3,924.0 4,129.7	417.8	3,712
72+00.00	25	115.6	7.9	5.8	106.9	9.7	5.3	4,129.7	429.9	3,807
72+25.00	25 25	123.9	7.9 6.5	6.2	110.7	6.7	5.5	4,236.6	438.3	3,909
72+30.00	50	123.9	5.4	6.2	229.8	11.0	11.5	4,547.2	450.5	3,909 4,125
73+50.00	50 50	124.4	5.4 5.1	6.3	232.0	9.7	11.6	4,809.1	464.1	4,125 4,345
74+00.00	50 50	120.2	9.3	6.1	229.6	13.4	11.5	5,038.7	480.8	4,558
74+50.00	50	117.8	11.9	5.9	221.8	19.6	11.1	5,260.5	505.4	4,755
75+00.00	50 50	137.7	15.4	6.9	236.5	25.3	11.8	5,497.0	537.0	4,755
75+50.00	50 50	141.7	17.7	7.1	258.7	30.6	12.9	5,755.6	575.3	5,180
75+85.00	35	122.5	0.0	6.1	171.3	11.5	8.6	5,926.9	589.6	5,337
76+00.00	15	132.5	0.0	6.6	70.8	0.0	3.5	5,997.7	589.6	5,408
76+50.00	50	135.5	0.0	6.8	248.1	0.0	12.4	6,245.9	589.7	5,656
77+00.00	50 50	145.6	8.8	7.3	260.3	8.2	13.0	6,506.2	600.0	5,906
77+50.00	50 50	142.9	8.4	7.1	267.2	15.9	13.4	6,773.4	619.9	6,153
78+00.00	50 50	139.7	5.1	7.0	261.6	12.6	13.1	7,035.0	635.6	6,399
78+50.00	50	140.7	1.9	7.0	259.6	6.5	13.0	7,294.6	643.8	6,651
78+55.00	5	131.0	1.7	6.6	25.2	0.3	1.3	7,319.7	644.2	6,676
78+75.00	20	129.1	1.7	6.5	96.3	1.2	4.8	7,416.1	645.7	6,770
79+00.00	25 25	133.5	2.5	6.7	121.6	1.9	6.1	7,537.6	648.1	6,890
79+50.00	50	129.3	3.2	6.5	243.3	5.2	12.2	7,781.0	654.6	7,126
80+00.00	50	132.4	2.5	6.6	242.3	5.2	12.1	8,023.2	661.1	7,362
80+50.00	50	114.9	0.5	5.7	229.0	2.8	11.4	8,252.2	664.6	7,588
80+75.00	25	113.5	0.0	5.7	105.8	0.2	5.3	8,358.0	664.9	7,693
81+00.00	25	97.5	0.0	4.9	97.7	0.0	4.9	8,455.7	664.9	7,791
81+50.00	50	123.0	0.6	6.1	204.1	0.5	10.2	8,659.8	665.5	7,994
82+00.00	50	109.0	0.0	5.4	214.8	0.5	10.7	8,874.5	666.2	8,208

NOTES:

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CON'T NEXT SHEET

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET E

9

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STH	33		AREA (S	F)	INCREMEN	TAL VOL (CY) (UN	IADJUSTED)	CUMULATIVE VOL (CY)		
07.17.01	5,05,1,1,05	6: II			CUT	FILL		CUT 1.00	EXPANDED FILL	MASS ORDINATE
STATION	DISTANCE	CUT	FILL	EB\$	NOTE 1	NOTE 3	EBS	NOTE 1	1.25	NOTE 4
82+50.00	50	116.5	0.0	5.8	208.8	0.0	10.4	9,083.3	666.2	8,417
83+00.00	50	133.6	0.8	6.7	231.6	0.8	11.6	9,314.9	667.1	8,648
83+25.00	25	130.4	0.0	6.5	122.2	0.4	6.1	9,437.1	667.6	8,770
83+50.00	25	136.0	1.3	6.8	123.4	0.6	6.2	9,560.5	668.3	8,892
84+00.00	50	133.8	2.0	6.7	249.9	3.0	12.5	9,810.3	672.1	9,138
84+15.00	15	134.7	0.0	6.7	74.6	0.6	3.7	9,884.9	672.8	9,212
84+50.00	35	137.0	2.9	6.9	176.1	1.9	8.8	10,061.1	675.1	9,386
84+60.00	10	134.8	0.0	6.7	50.3	0.5	2.5	10,111.4	675.8	9,436
85+00.00	40	131.0	2.5	6.5	196.8	1.8	9.8	10,308.2	678.1	9,630
85+30.00	30	132.6	0.0	6.6	146.4	1.4	7.3	10,454.6	679.8	9,775
85+50.00	20	131.2	5.9	6.6	97.7	2.2	4.9	10,552.3	682.5	9,870
86+00.00	50	133.3	3.6	6.7	244.9	8.8	12.2	10,797.2	693.5	10,104
86+50.00	50	129.6	0.0	6.5	243.4	3.4	12.2	11,040.6	697.7	10,343
87+00.00	50	129.9	5.0	6.5	240.2	4.6	12.0	11,280.9	703.6	10,577
87+50.00	50	127.7	5.3	6.4	238.5	9.5	11.9	11,519.4	715.4	10,804
88+00.00	50	126.4	0.0	6.3	235.2	4.9	11.8	11,754.6	721.5	11,033
88+50.00	50	139.4	1.0	7.0	246.1	0.9	12.3	12,000.7	722.7	11,278
89+00.00	50	143.4	0.7	7.2	261.9	1.6	13.1	12,262.6	724.6	11,538
89+50.00	50	137.2	0.7	6.9	259.8	1.3	13.0	12,522.4	726.3	11,796
90+00.00	50	114.6	0.9	5.7	233.1	1.5	11.7	12,755.5	728.1	12,027
90+30.00	30	109.8	0.0	5.5	124.7	0.5	6.2	12,880.1	728.7	12,151
90+50.00	20	137.4	1.4	6.9	91.5	0.5	4.6	12,971.7	729.3	12,242
91+00.00	50	131.7	1.5	6.6	249.1	2.7	12.5	13,220.8	732.7	12,488
91+50.00	50	112.9	8.6	5.6	226.5	9.4	11.3	13,447.3	744.4	12,703
92+00.00	50	106.5	6.3	5.3	203.1	13.8	10.2	13,650.5	761.7	12,889
92+40.00	40	102.0	0.0	5.1	154.4	4.6	7.7	13,804.9	767.4	13,037
92+50.00	10	103.5	2.6	5.2	38.1	0.5	1.9	13,842.9	768.1	13,075
93+00.00	50	103.6	9.1	5.2	191.7	10.8	9.6	14,034.6	781.6	13,253
93+20.00	20	96.3	8.2	4.8	74.0	6.4	3.7	14,108.7	789.6	13,319
93+50.00	30	109.0	2.1	5.5	114.1	5.7	5.7	14,222.7	796.7	13,426
93+70.00	20	114.9	0.0	5.7	82.9	0.8	4.1	14,305.6	797.7	13,508
94+00.00	30	128.3	0.6	6.4	135.1	0.3	6.8	14,440.7	798.1	13,643
94+50.00	50	122.6	0.0	6.1	232.3	0.5	11.6	14,673.1	798.7	13,874
95+00.00	50	133.8	2.3	6.7	237.4	2.1	11.9	14,910.5	801.4	14,109
95+50.00	50	139.9	0.5	7.0	253.5	2.6	12.7	15,163.9	804.6	14,359
95+75.00	25	126.9	0.0	6.3	123.5	0.2	6.2	15,287.5	804.9	14,483
96+00.00	25	127.3	1.0	6.4	117.7	0.5	5.9	15,405.2	805.5	14,600
96+50.00	50	82.2	3.3	4 .1	194.0	4.0	9.7	15,599.1	810.5	14,789
96+75.00	25	78.0	0.0	3.9	74.2	1.5	3.7	15,673.3	812.3	14,861
97+00.00	25	96.0	3.1	4.8	80.6	1.4	4.0	15,753.9	814.1	14,940

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CON'T NEXT SHEET

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET E

STH	STH 33		AREA (SF)			TAL VOL (CY) (UN	(ADJUSTED)	CUMULATIVE VOL (CY)		
STATION	DISTANCE	415	=	9	CUT	FILL	500	CUT 1.00	EXPANDED FILL	MASS ORDINATE
STATION	DISTANCE	CUT	FILL	EBS	NOTE 1	NOTE 3	EBS	NOTE 1	1.25	NOTE 4
97+50.00	50 50	97.9	2.3	4.9	179.5	5.0	9.0	15,933.4	820.4	15,113
98+00.00	50	103.9	1.9	5.2	186.9	3.9	9.3	16,120.3	825.3	15,295
98+50.00	50 50	108.3	0.0	5.4	196.5	1.8	9.8	16,316.7	827.5	15,489
99+00.00		112.9	1.9	5.6	204.7	1.8	10.2	16,521.5	829.7	15,692
99+50.00	50 50	112.7	5.9	5.6	208.9	7.3	10.4	16,730.3	838.8	15,891
100+00.00	50 50	236.7	0.0 3.5	11.8	323.5	5.5	16.2 16.7	17,053.9	845.7	16,208
100+50.00	50	124.0		6.2	333.9	3.3		17,387.8	849.8	16,538
101+00.00	50 50	140.0	0.0	7.0	244.4	3.3	12.2	17,632.2	853.8	16,778
101+50.00	50 30	130.1	0.1	6.5	250.0	0.1	12.5	17,882.2	853.9	17,028
101+80.00	30	112.8	0.3	5.6	134.9	0.2	6.7	18,017.1	854.2	17,163
102+00.00	20	120.4	0.5	6.0	86.4	0.3	4.3	18,103.4	854.6	17,249
102+30.00 102+50.00	30	121.0 128.1	0.0 0.1	6.1 6.4	134.1 92.3	0.3 0.0	6.7 4.6	18,237.6	855.0	17,383
	20						1	18,329.9	855.0	17,475
102+70.00 103+00.00	20 30	120.6 115.8	0.4 0.3	6.0 5.8	92.1 131.4	0.2 0.4	4.6 6.6	18,422.0 18,553.4	855.2 855.7	17,567
103+30.00	30	107.3		5.4				18,677.3	856.2	17,698
			0.4		124.0 77.7	0.4	6.2			17,821
103+50.00	20 45	102.6	0.9	5.1 5.0		0.5	3.9	18,755.0	856.8	17,898
103+95.00	45 5	99.9	0.2	5.0	168.7	0.9	8.4	18,923.7	857.9	18,066
104+00.00		100.6	0.2	5.0	18.6	0.0	0.9	18,942.3	857.9	18,084
104+50.00 104+60.00	50 10	118.4 118.2	0.2	5.9 5.9	202.8 43.8	0.4 0.0	10.1	19,145.1 19,188.9	858.4	18,287
			0.0					· ·	858.5	18,330
104+75.00	15	122.3	0.0 0.0	6.1 6.9	66.8	0.0 0.0	3.3 6.0	19,255.7 19,376.0	858.5	18,397
105+00.00 105+25.00	25 25	137.5			120.2	1	1		858.5	18,517
105+38.99	14	129.8 132.5	0.0 0.0	6.5 6.6	123.7 68.0	0.0 0.0	6.2 3.4	19,499.7 19,567.6	858.5 858.5	18,641
105+50.00	11	134.6	0.0	6.7	54.5	0.0	2.7	19,567.6	858.5	18,709 18,764
106+00.00		121.7	2.1		237.4	1	11.9	19,859.4	860.9	·
106+20.00	50 20	106.9	0.0	6.1 5.3	237.4 84.7	1.9 0.8	4.2	19,059.4	861.9	18,999 19,082
106+50.00	30	103.9	0.0	5.3 5.2	0 4 .7 117.1	0.0	5.9	20,061.2	861.9	19,082
107+00.00	50	111.0	0.9	5.5	198.9	0.8	9.9	20,260.1	862.9	19,397
107+50.00	50	100.0	1.3	5.0	195.3	2.1	9.8	20,455.5	865.5	19,590
107+70.00	20	96.9	0.7	4.8	72.9	0.7	3.6	20,435.5	866.4	19,662
108+00.00	30	94.9	2.0	4.6 4.7	106.5	1.5	5.3	20,526.4	868.3	19,767
108+30.00	30	88.0	1.1	4.7	101.6	1.7	5.1	20,634.9	870.4	19,866
108+50.00	20	85.6	2.9	4.3	64.3	1.5	3.2	20,730.3	872.3	19,929
109+00.00	50	83.1	3.2	4.2	156.2	5.7	7.8	20,000.8	879.4	20,078
109+50.00	50	94.3	1.4	4.7	164.3	4.3	8.2	21,121.3	884.7	20,078
109+70.77	21	100.1	0.3	5.0	74.8	0.6	3.7	21,121.3	885.6	20,237
110+00.00	29	100.1	0.3	5.5	113.3	0.0	5.7	21,190.0	885.8	20,424
110+50.00	50	120.0	0.9	6.0	212.3	0.9	10.6	21,509.4	887.0	20,635
110700.00	1 00	120.0	0.8	0.0	212.0	J 0.8	10.0	21,021.0	007.0	20,000

NOTES:

- 1) CUT: CUT INCLUDES UNUSABLE PAVEMENT MATERIAL
- 2) FILL: FILL DOES NOT INCLUDE UNUSABLE PAVEMENT MATERIAL
- 3) MASS ORDINATE: MASS ORDINATE = (CUT UNUSABLE PAVEMENT MATERIAL) (FILL * FILL FACTOR)

CON'T NEXT SHEET

PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET **E**

STH 33		AREA (S	F)	INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)			
STATION	DISTANCE	CUT	FILL	EBS	CUT NOTE 1	FILL NOTE 3	EBS	CUT 1.00 NOTE 1	EXPANDED FILL 1.25	MASS ORDINATE NOTE 4
110+85.00	35	117.1	0.0	5.9	153.7	0.6	7.7	21,675.3	887.7	20,788
111+00.00	15	114.4	0.0	5.7	64.3	0.0	3.2	21,739.6	887.7	20,852
111+50.00	50	108.6	0.6	5.4	206.4	0.6	10.3	21,946.1	888.4	21,058
111+63.46	13	103.8	0.8	5.2	52.9	0.4	2.6	21,999.0	888.8	21,110
112+00.00	37	82.7	0.0	4.1	126.2	0.5	6.3	22,125.1	889.5	21,236
112+50.00	50	90.2	3.2	4.5	160.0	3.0	8.0	22,285.2	893.2	21,392
113+00.00	50	96.3	1.3	4.8	172.6	4.2	8.6	22,457.8	898.5	21,559
113+50.00	50	94.8	1.4	4.7	176.9	2.5	8.8	22,634.7	901.6	21,733
114+00.00	50	92.7	0.0	4.6	173.7	1.3	8.7	22,808.4	903.2	21,905
114+50.00	50	89.7	2.3	4.5	168.9	2.2	8.4	22,977.2	905.9	22,071
115+00.00	50	89.3	2.3	4.5	165.7	4.3	8.3	23,142.9	911.3	22,232
115+50.00	50	89.1	0.0	4.5	165.2	2.2	8.3	23,308.1	914.1	22,394
116+00.00	50	86.3	0.2	4.3	162.4	0.2	8.1	23,470.5	914.3	22,556
116+00.31	0	86.3	0.2	4.3	1.0	0.0	0.0	23,471.5	914.3	22,557
COLUMN TOTALS		23,471.5	731.4	1,173.6						

NOTES:

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MULTI-USE PATH		AREA (S	F)	INCREMENT	AL VOL (CY) (L	JNADJUSTED)	CUMULAT			
STATION	DISTANCE	CUT	FILL	EBS	CUT NOTE 1	FILL NOTE 3	EBS	CUT 1.00 NOTE 1	EXPANDED FILL 1.25	MASS ORDINATE NOTE 4
58+44.69 PH		7.4	0.5	0.4				0.0	0.0	0
58+50.00 PH	5	7.4	0.5	0.4	1.5	0.1	0.1	1.5	0.1	1
59+00.00 PH	50	17.5	0.0	0.9	23.1	0.5	1.2	24.6	0.7	24
60+00.00 PH	100	2.2	3.0	0.1	36.5	5.6	1.8	61.1	7.8	53
61+00.00 PH	100	15.3	0.0	0.8	32.4	5.6	1.6	93.5	14.8	79
62+00.00 PH	100	46.6	0.0	2.3	114.7	0.0	5.7	208.2	14.8	193
63+00.00 PH	100	39.8	0.0	2.0	160.1	0.0	8.0	368.2	14.8	353
64+00.00 PH	100	18.2	5.2	0.9	107.5	9.7	5.4	475.7	26.9	449
65+00.00 PH	100	17.2	0.5	0.9	65.6	10.6	3.3	541.4	40.2	501
66+00.00 PH	100	2.4	3.5	0.1	36.4	7.4	1.8	577.7	49.5	528
66+13.52 PH	14	2.4	3.5	0.1	1.2	1.7	0.1	578.9	51.6	527
		COLUI	MN TOTALS	578.9	41.3	28.9				

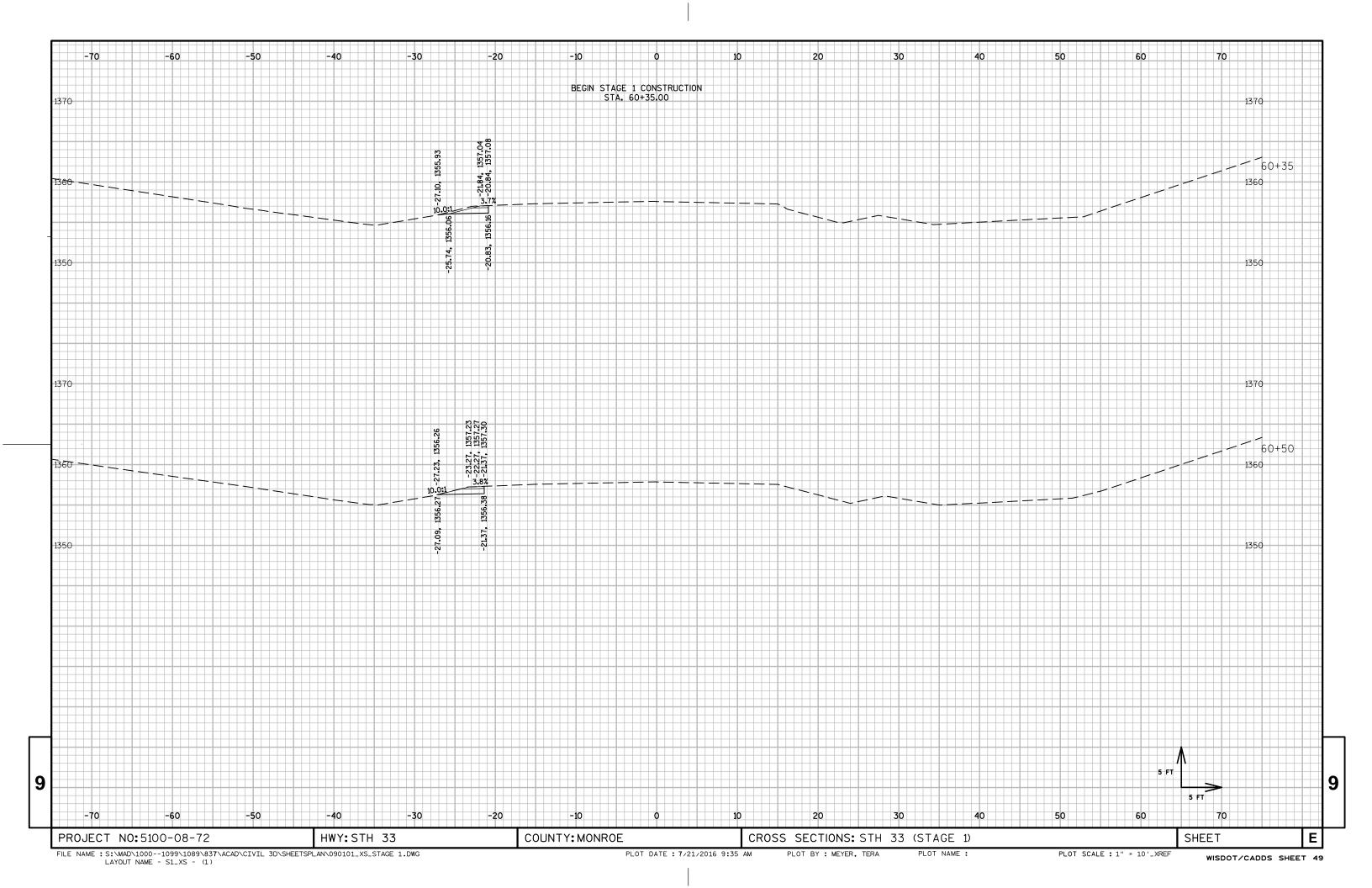
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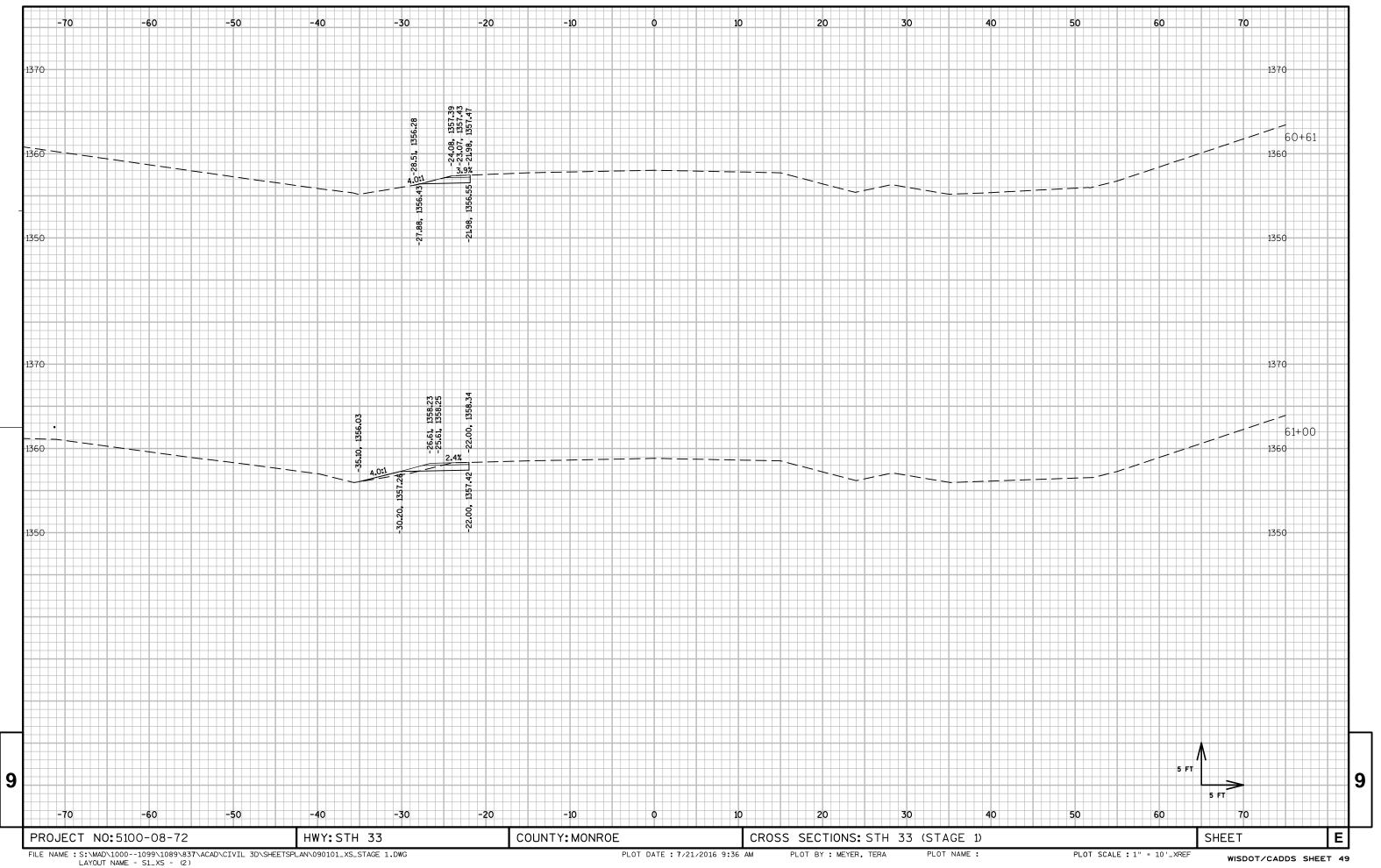
- 1) CUT: CUT INCLUDES UNUSABLE PAVEMENT MATERIAL
- 2) FILL: FILL DOES NOT INCLUDE UNUSABLE PAVEMENT MATERIAL
- 3) MASS ORDINATE: MASS ORDINATE = (CUT UNUSABLE PAVEMENT MATERIAL) (FILL * FILL FACTOR)

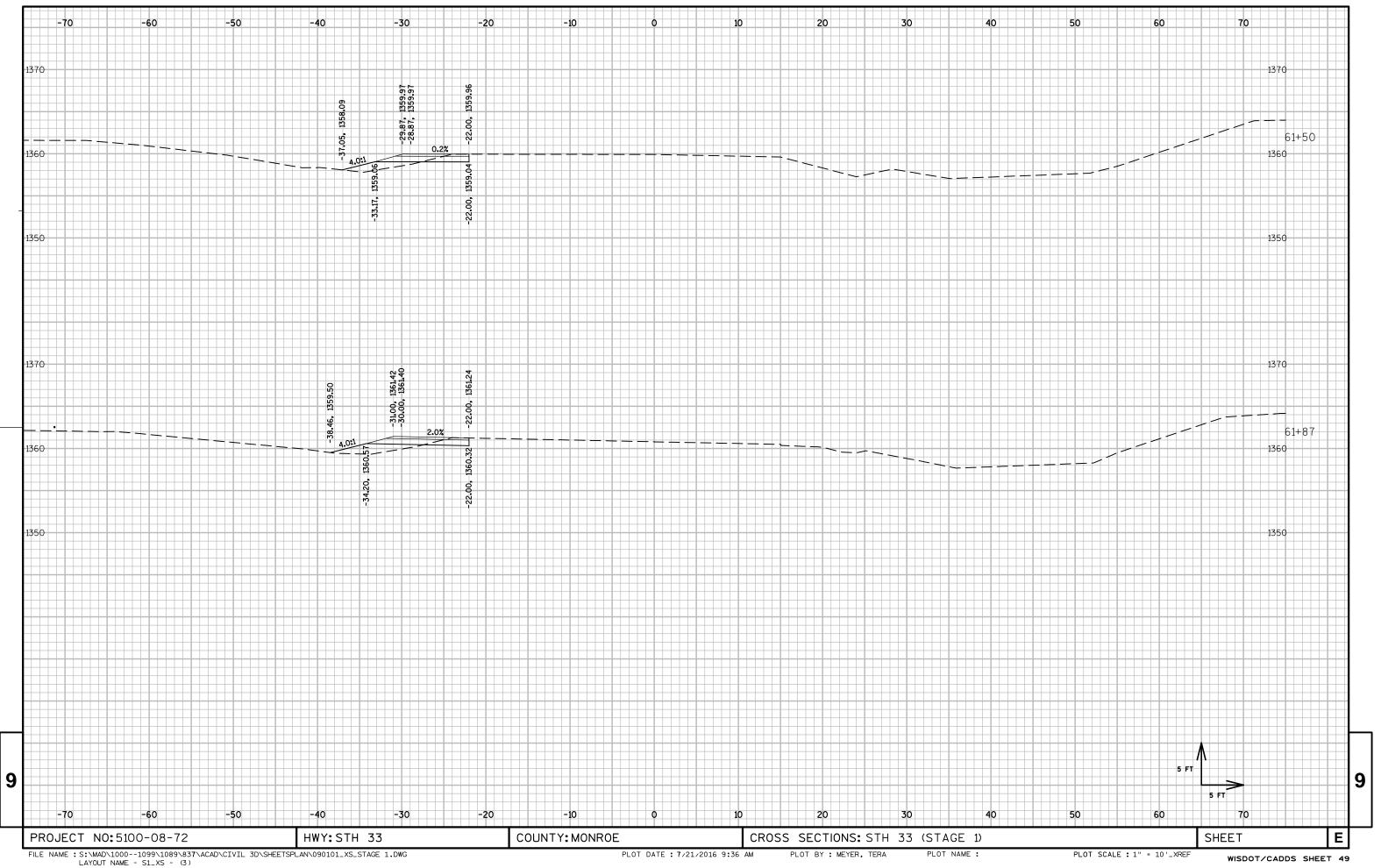
PROJECT NO:5100-08-72 HWY:STH 33 COUNTY:MONROE EARTHWORK SHEET **E**

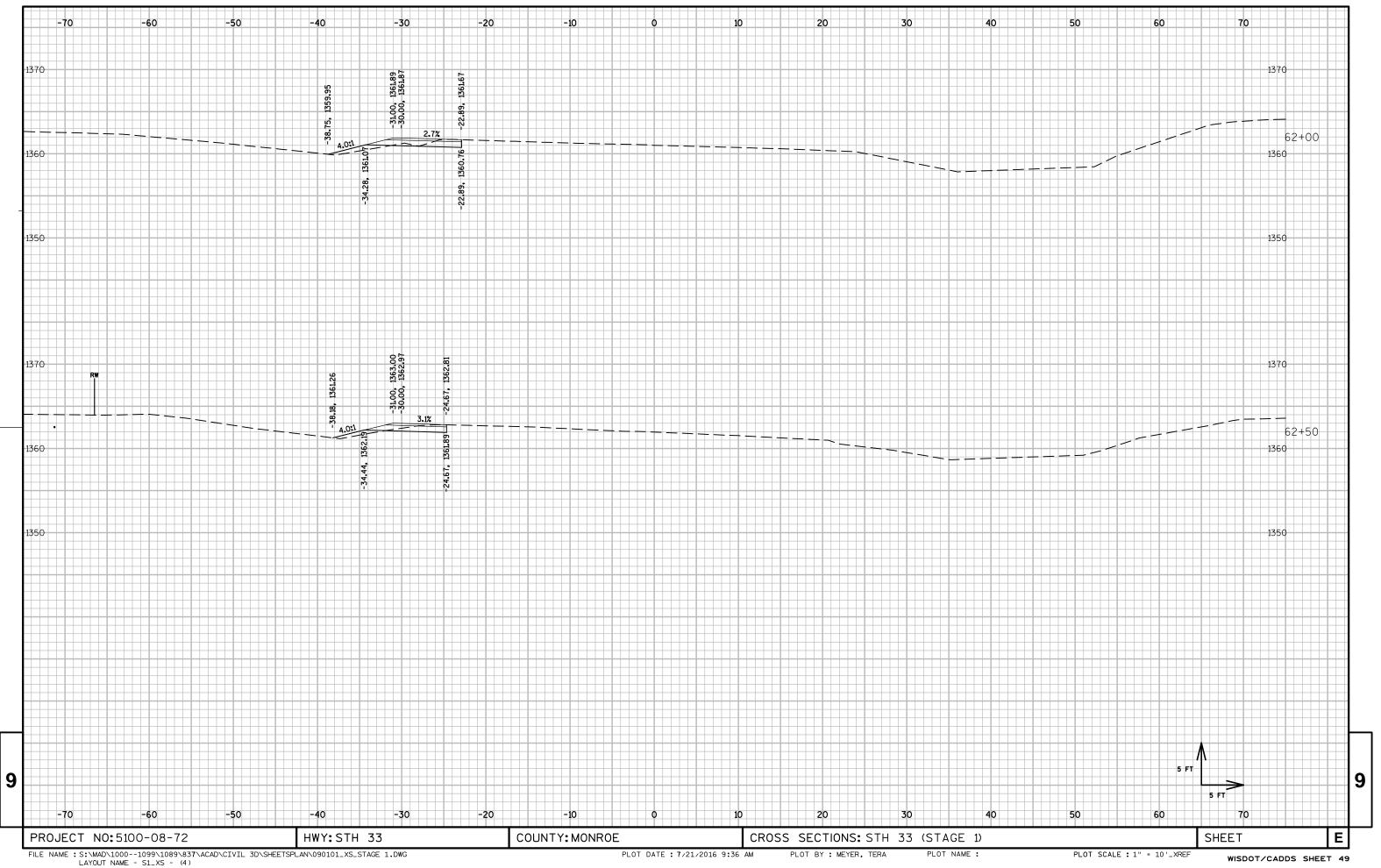
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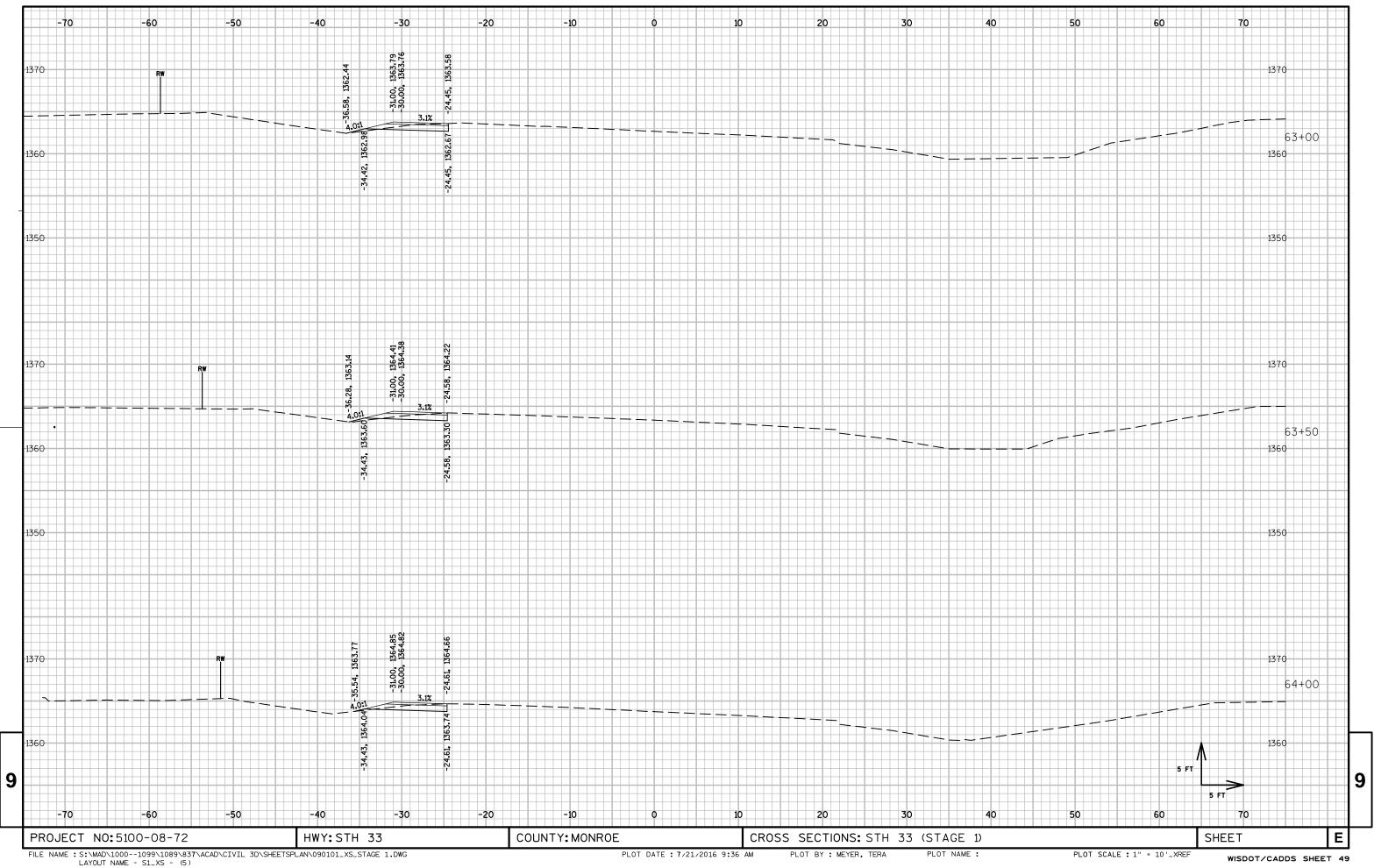
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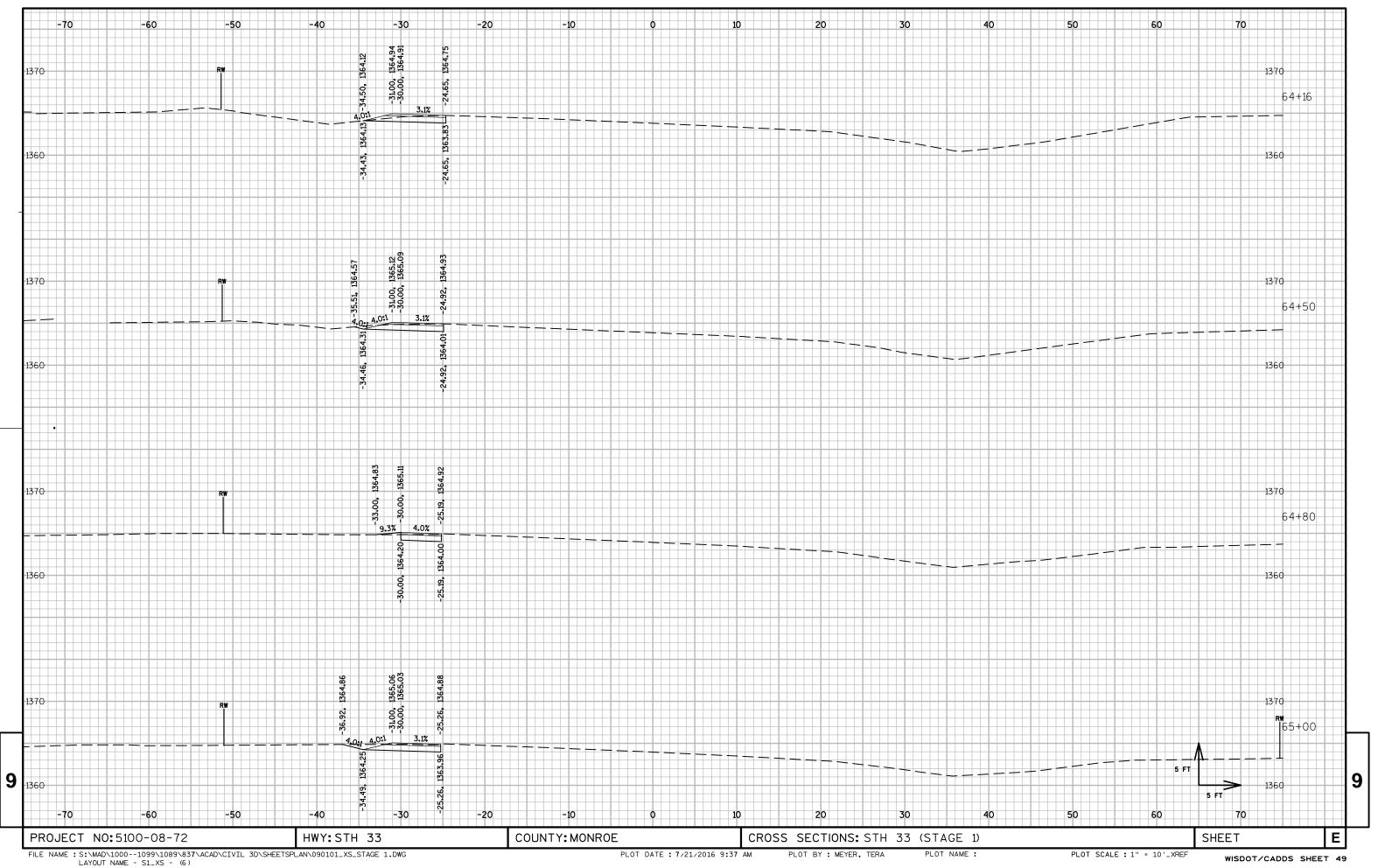


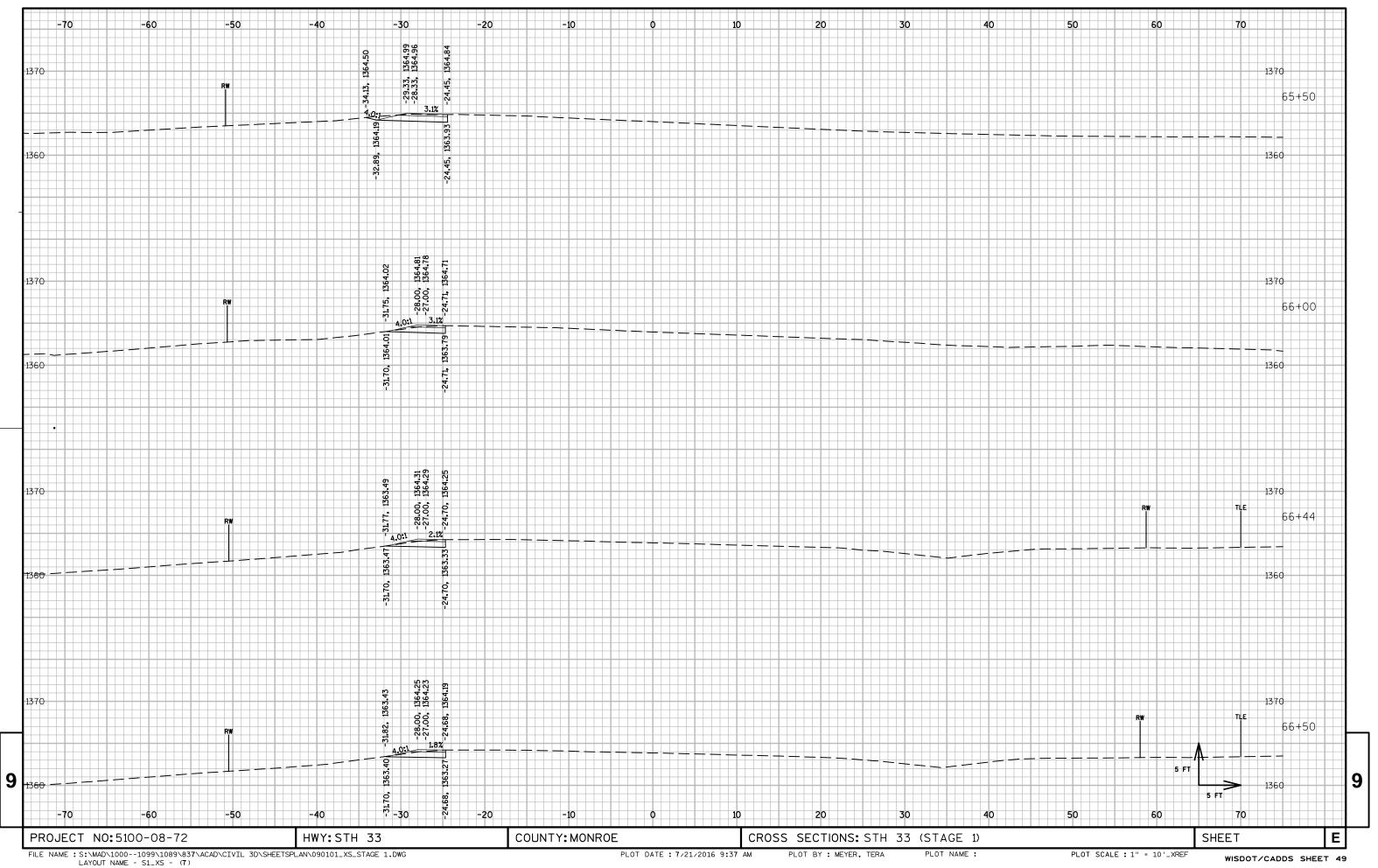


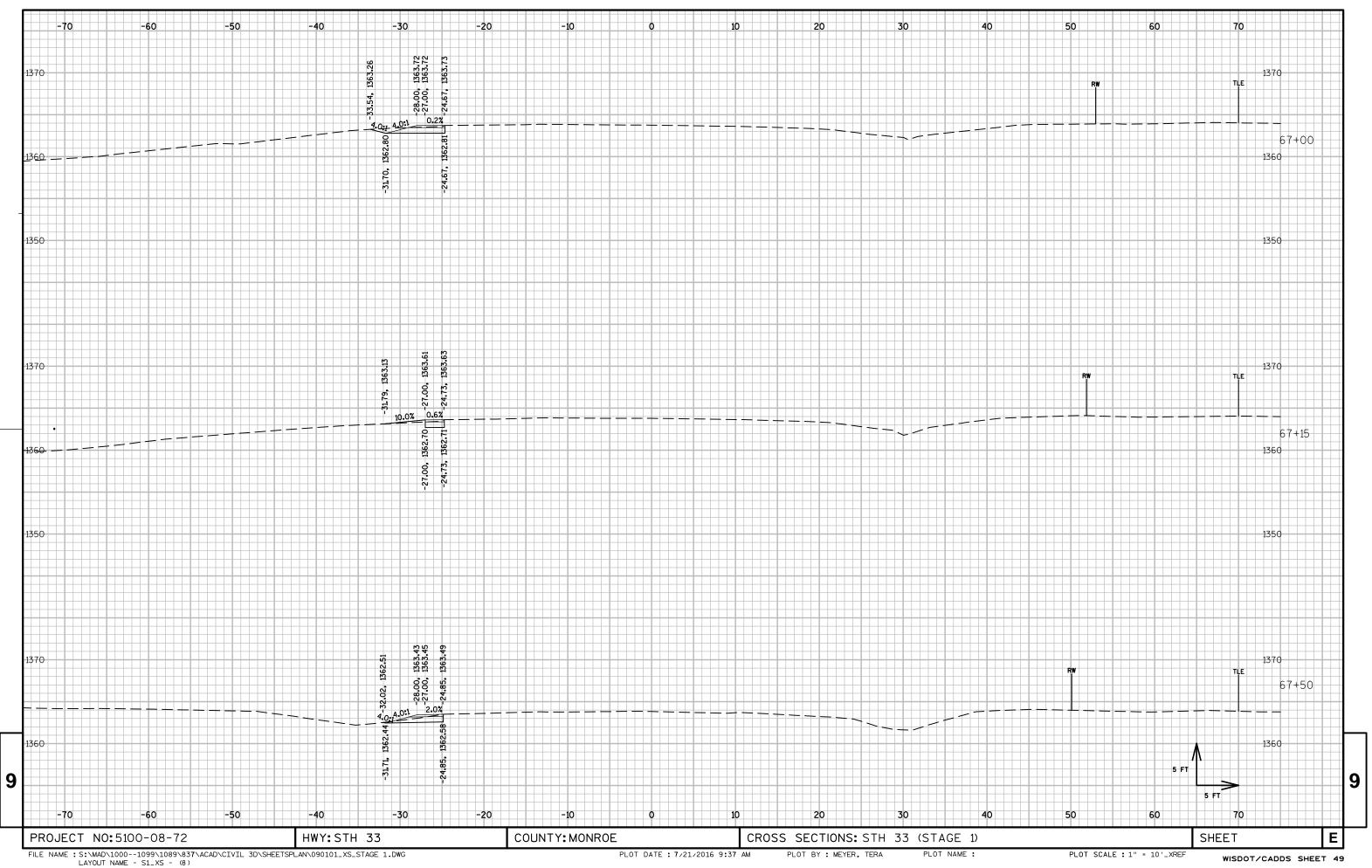


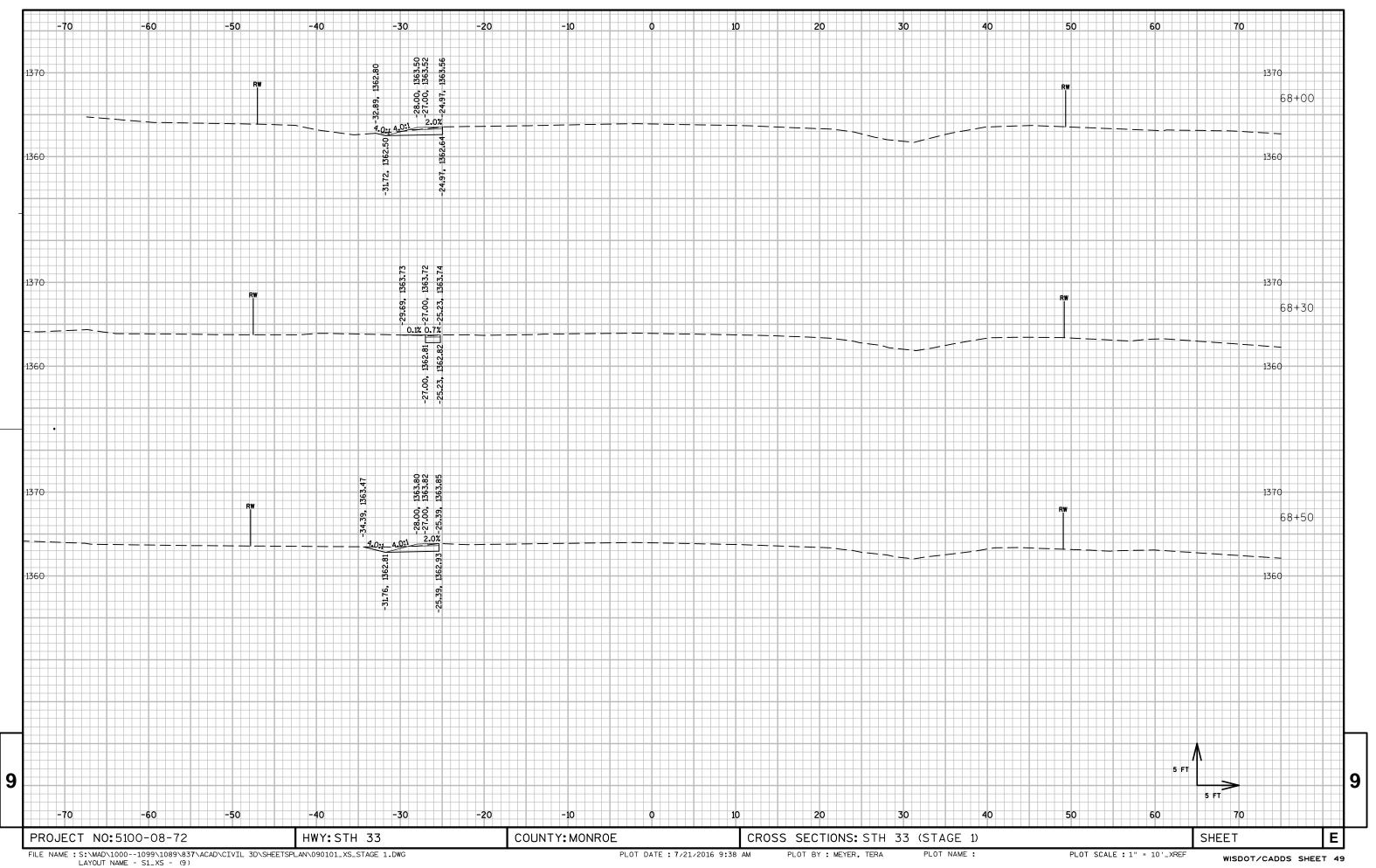


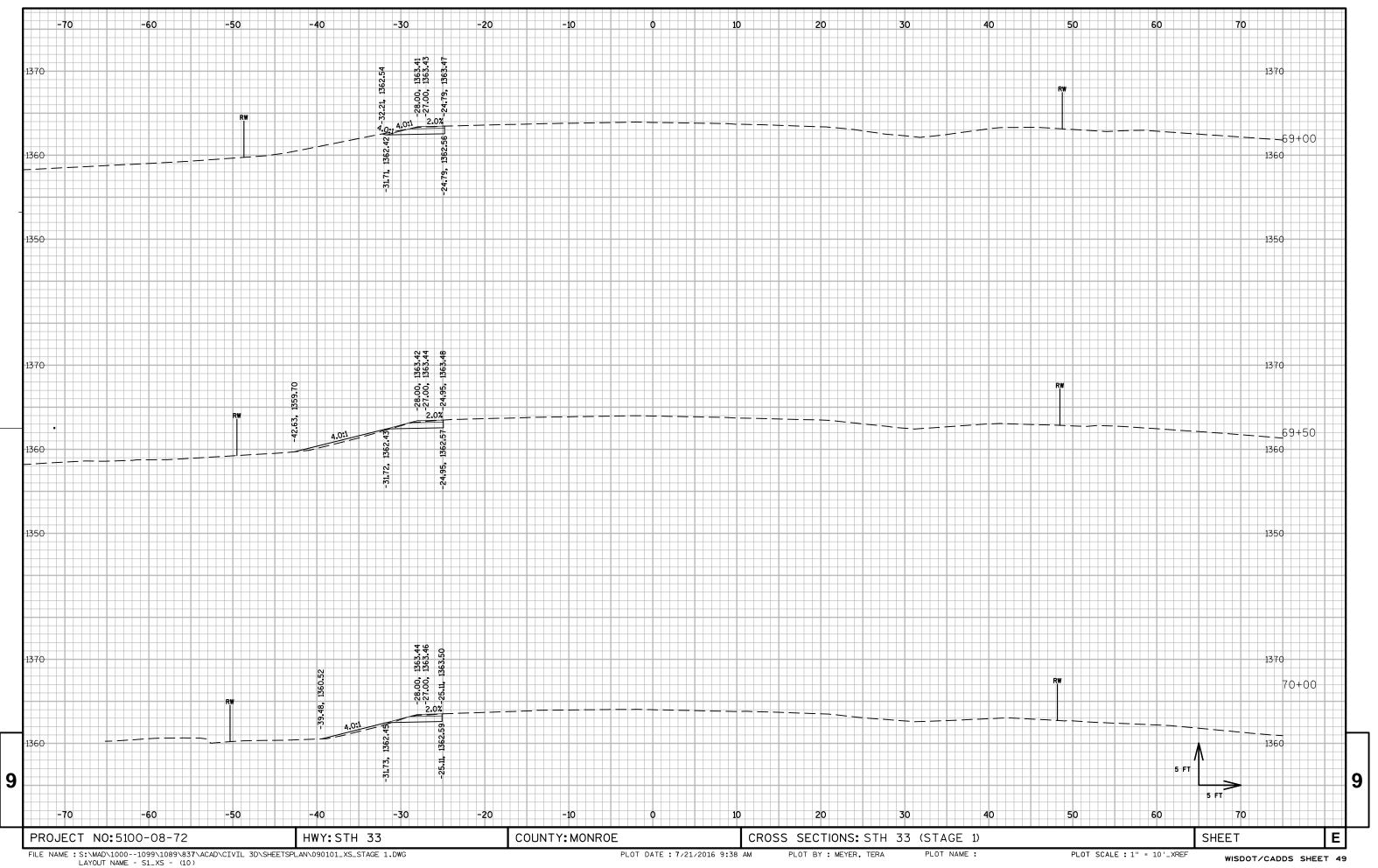


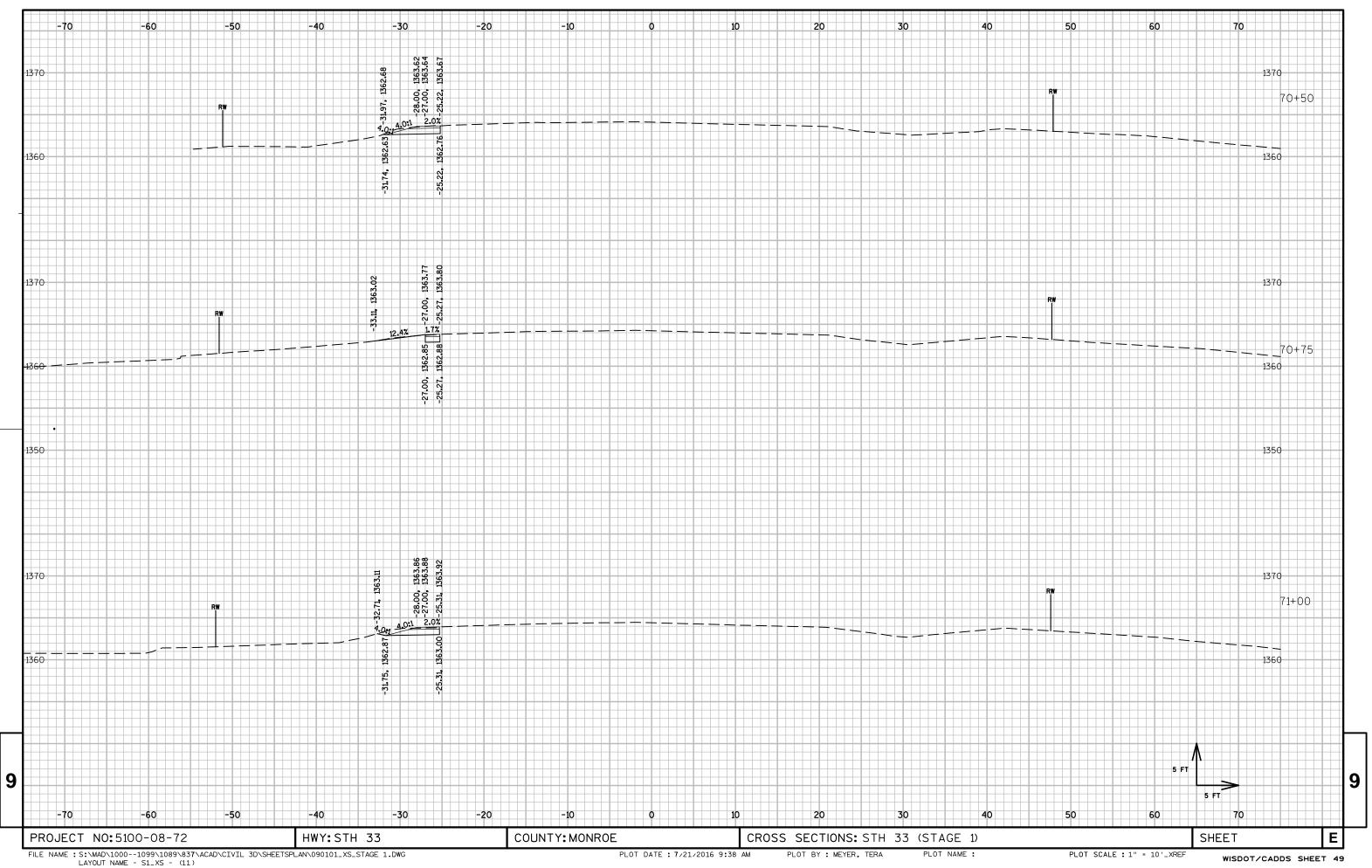


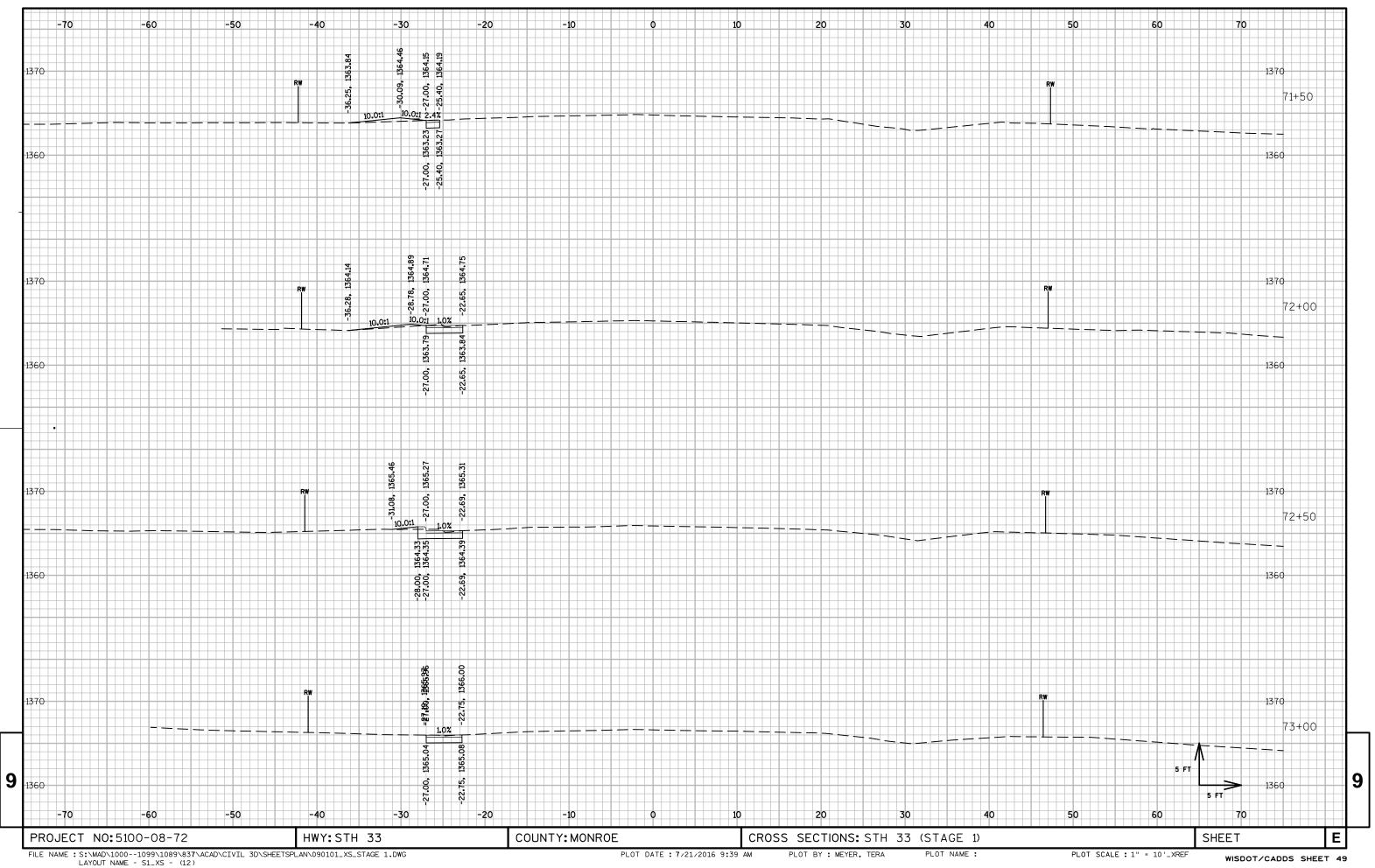


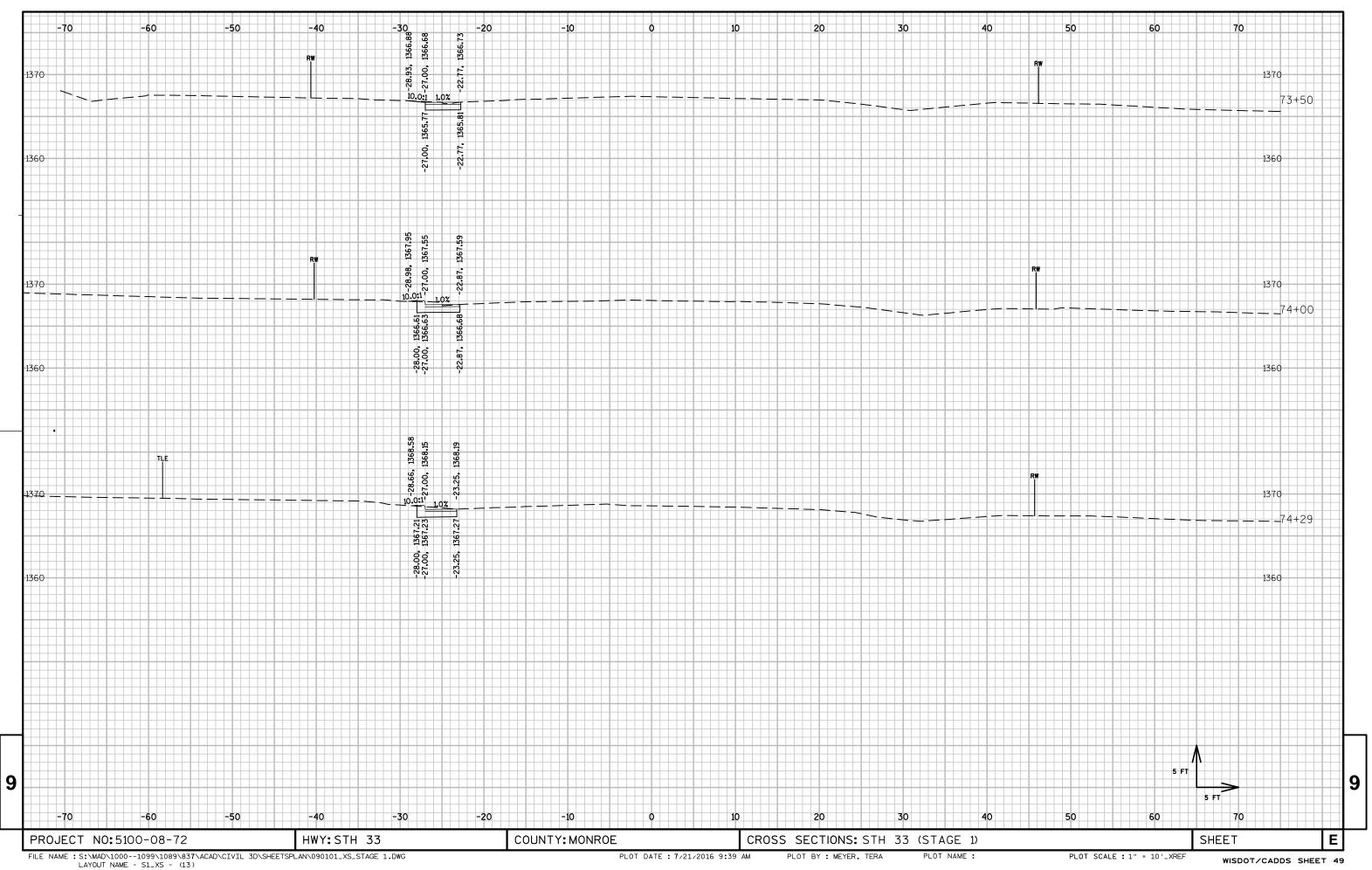


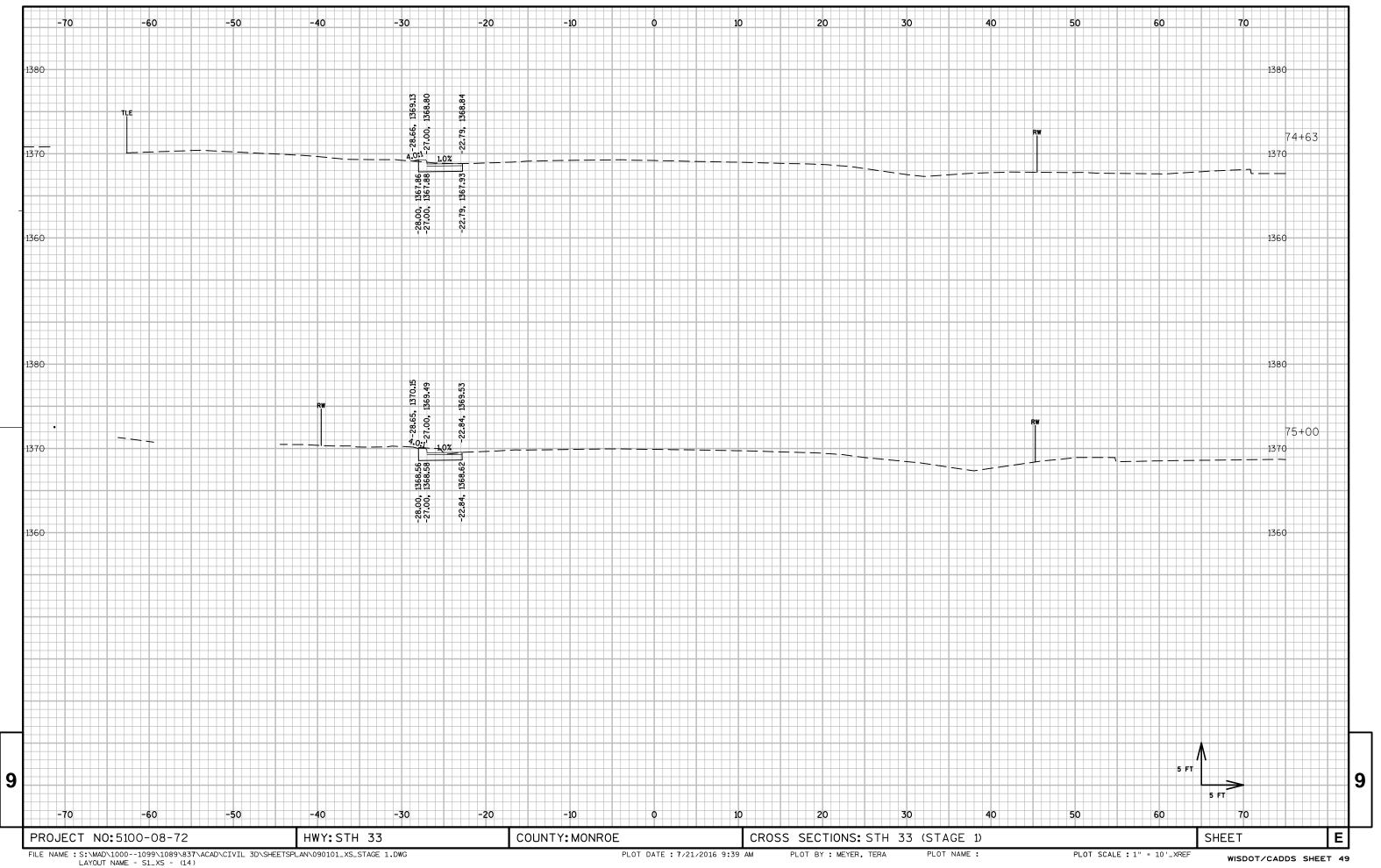


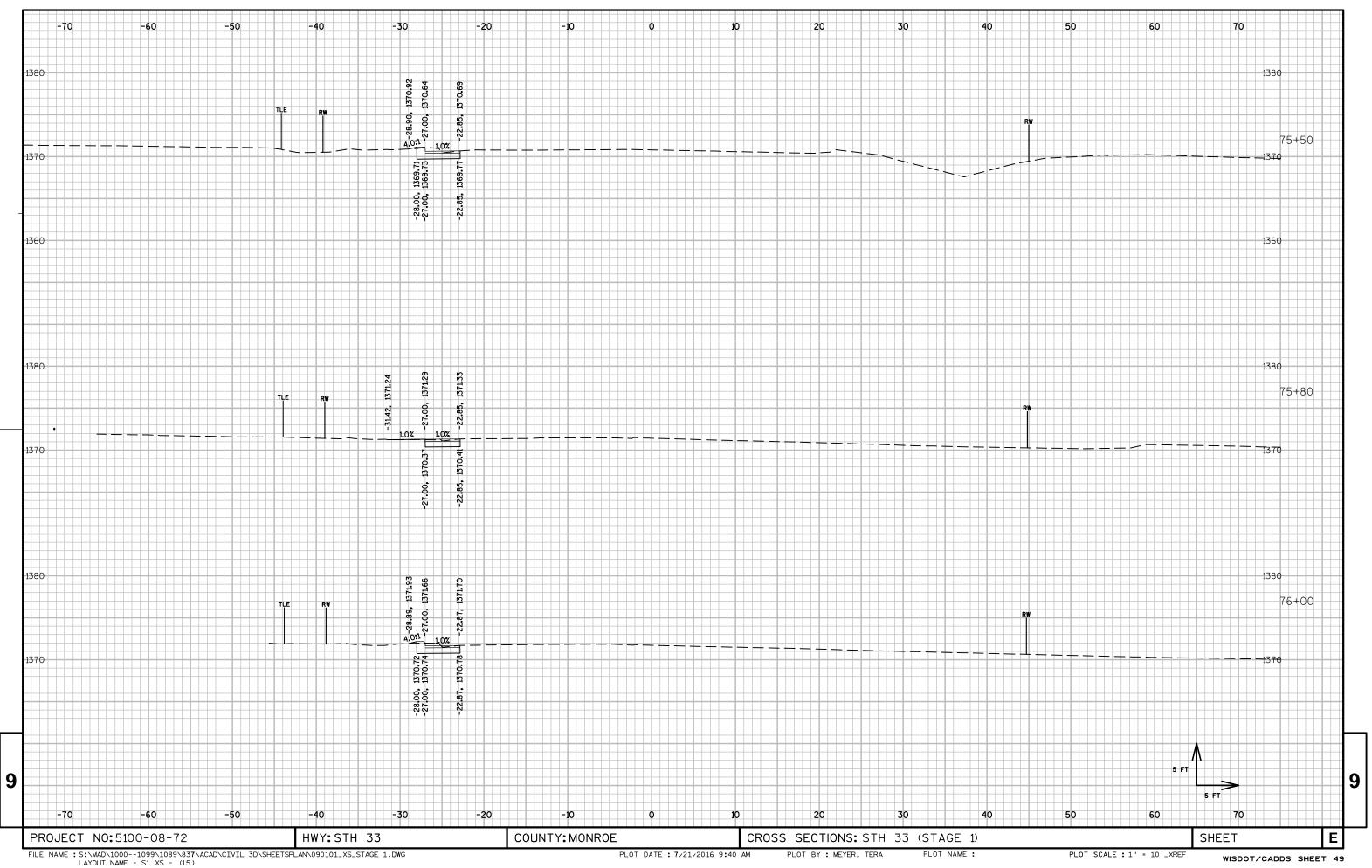


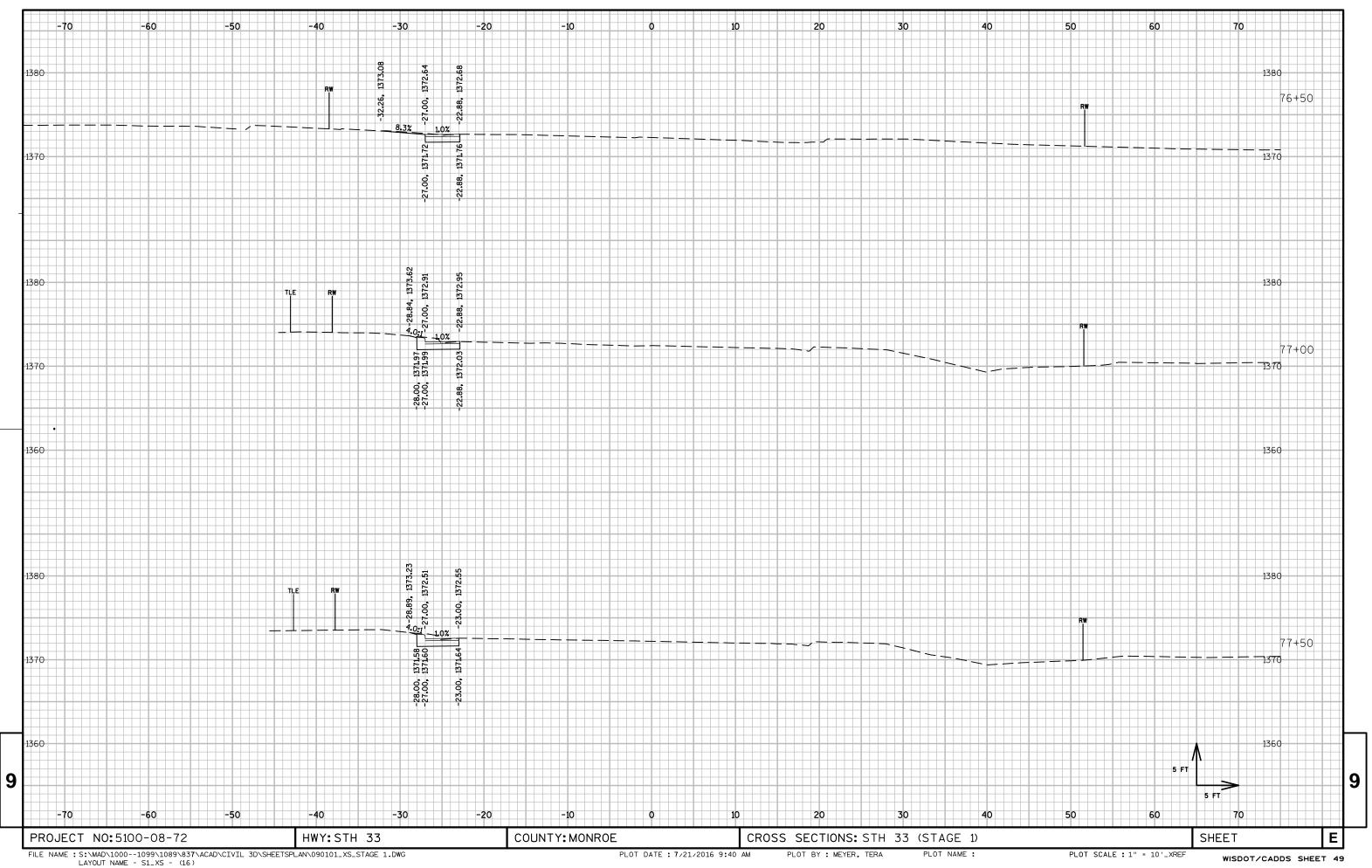


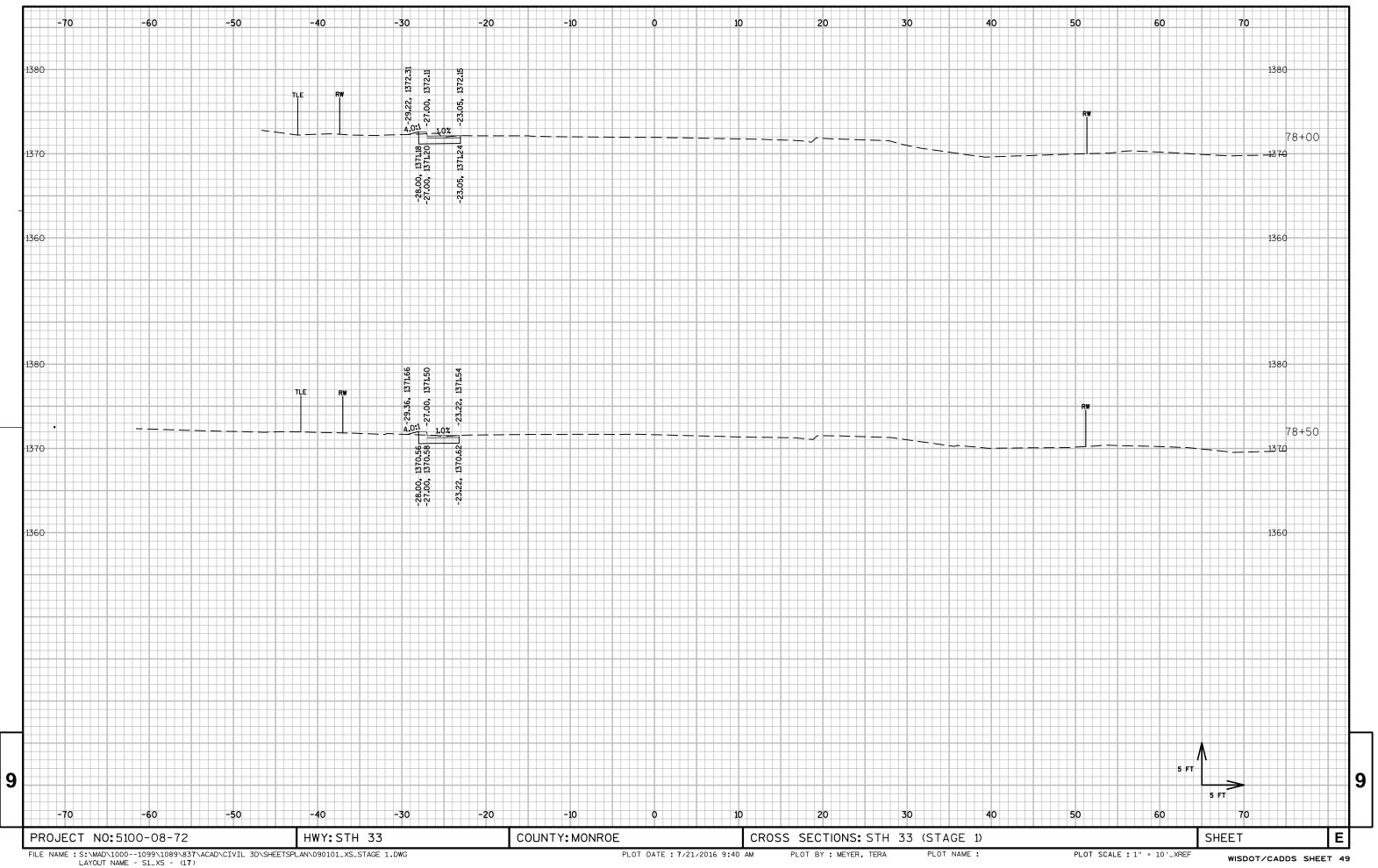


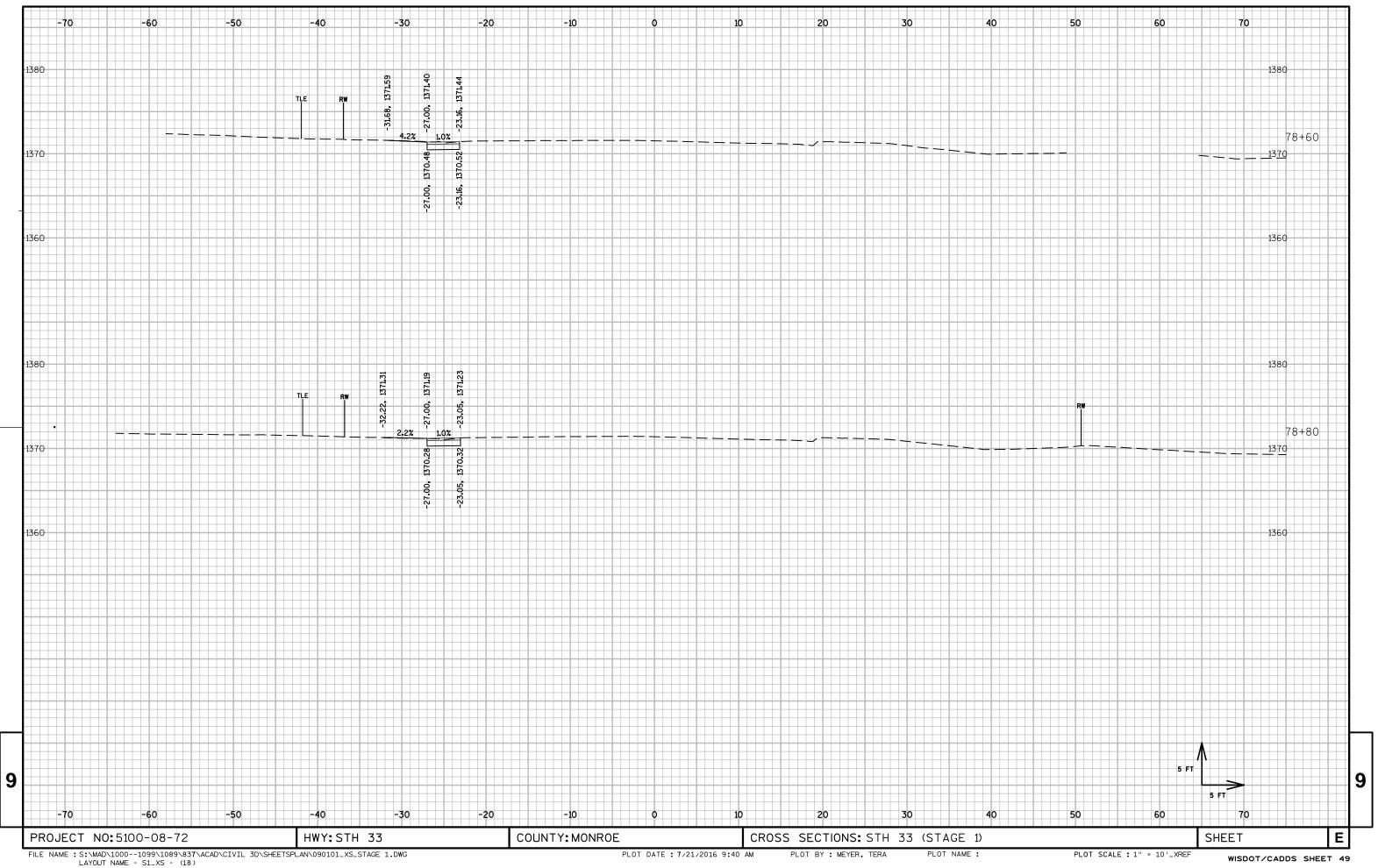


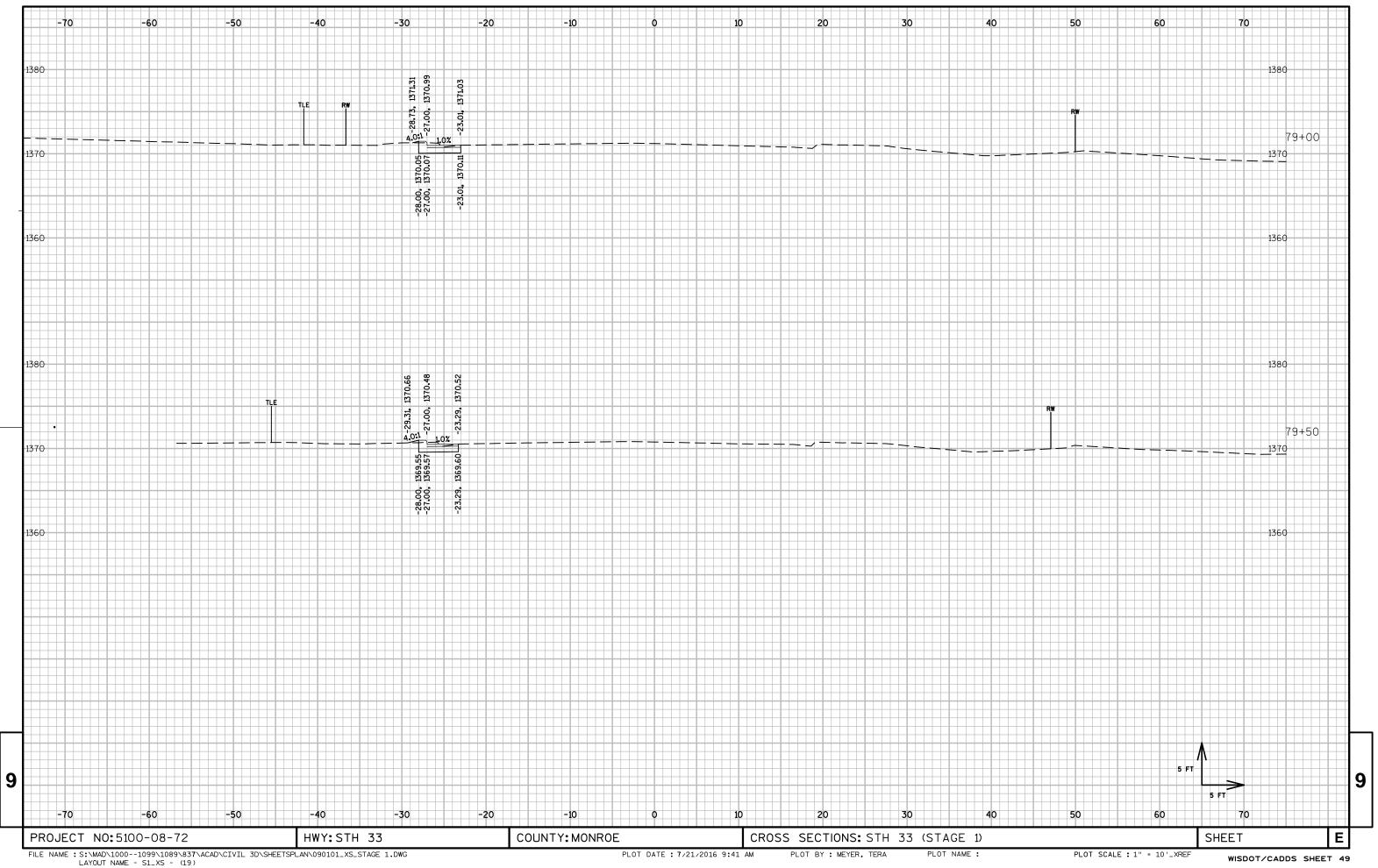


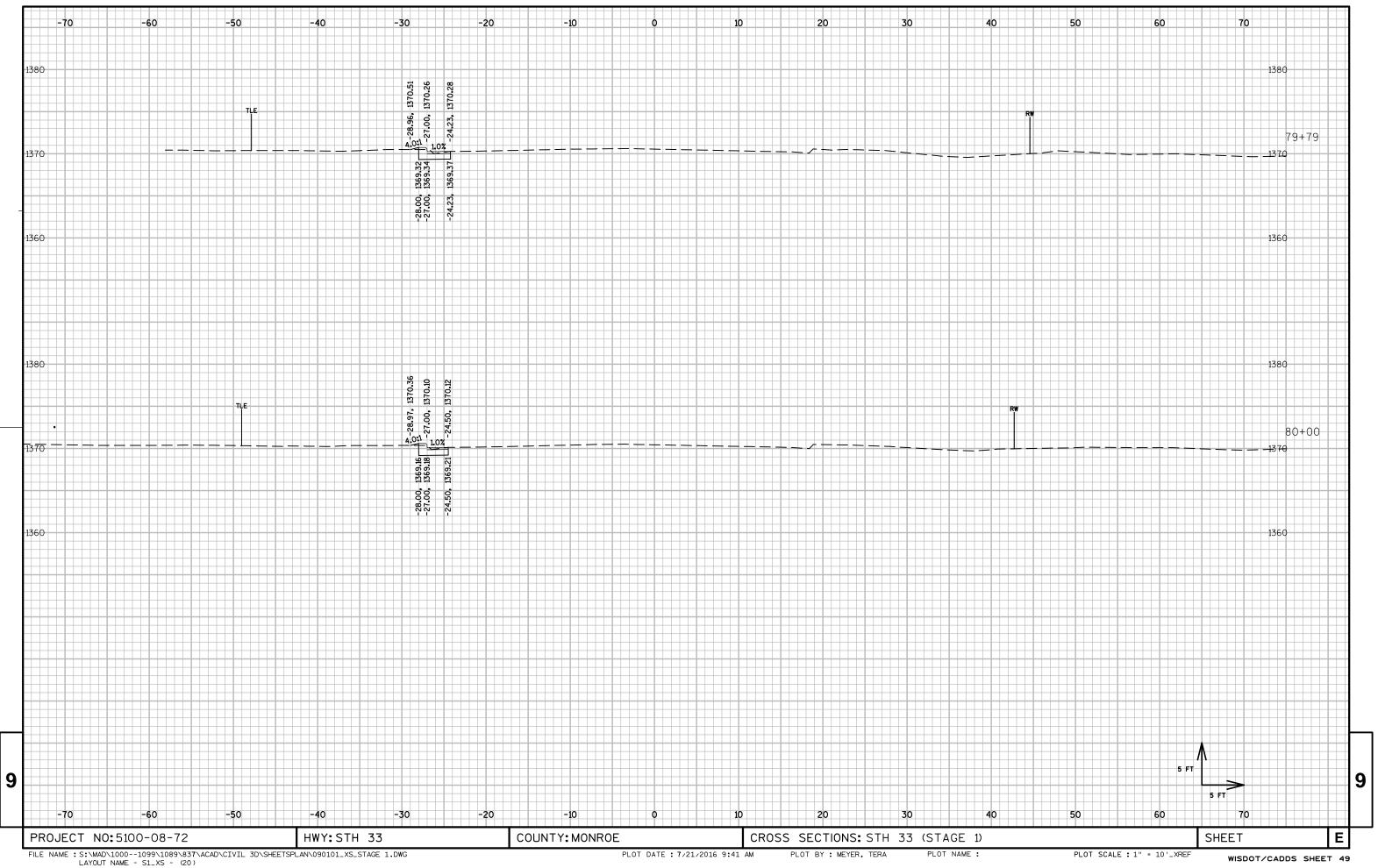


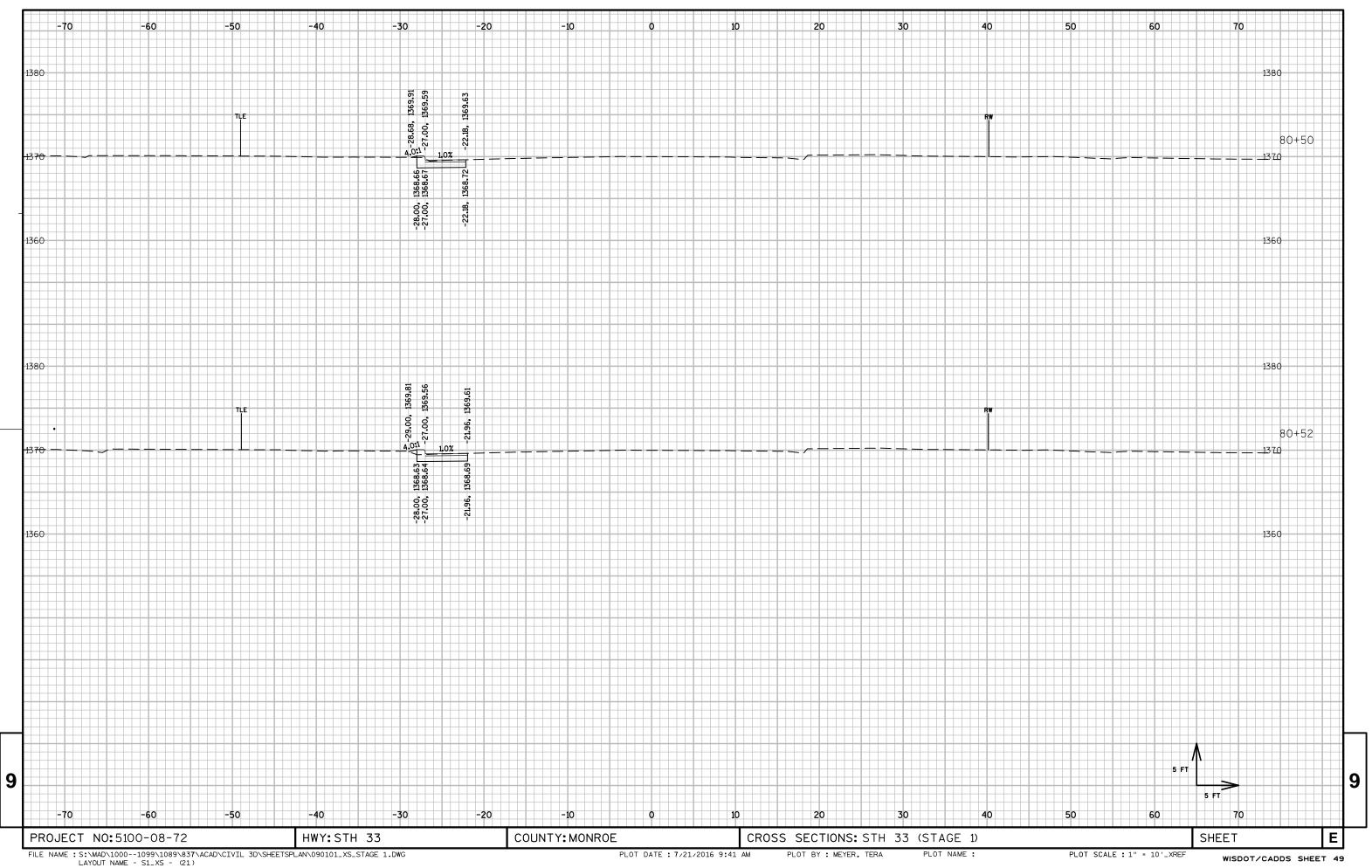


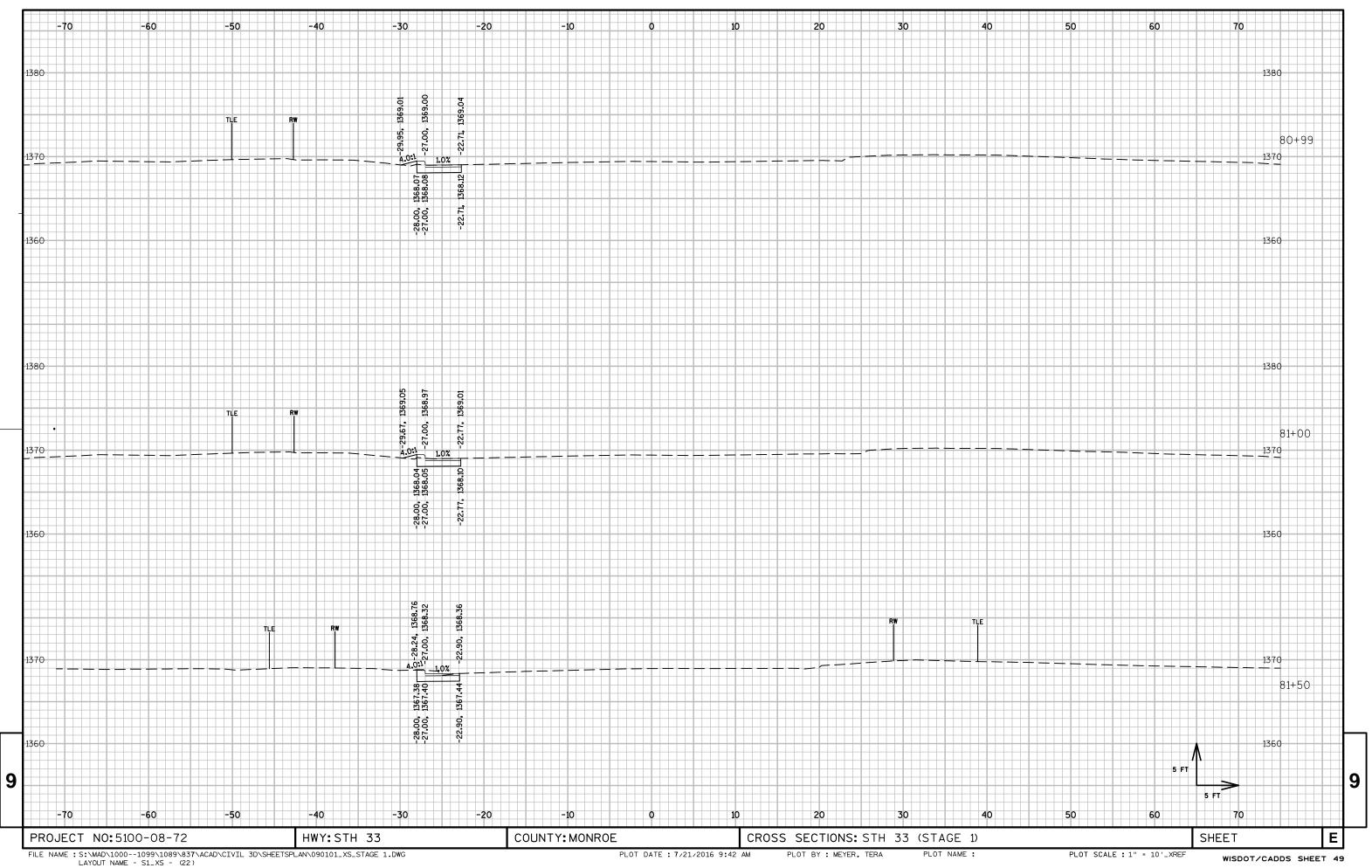


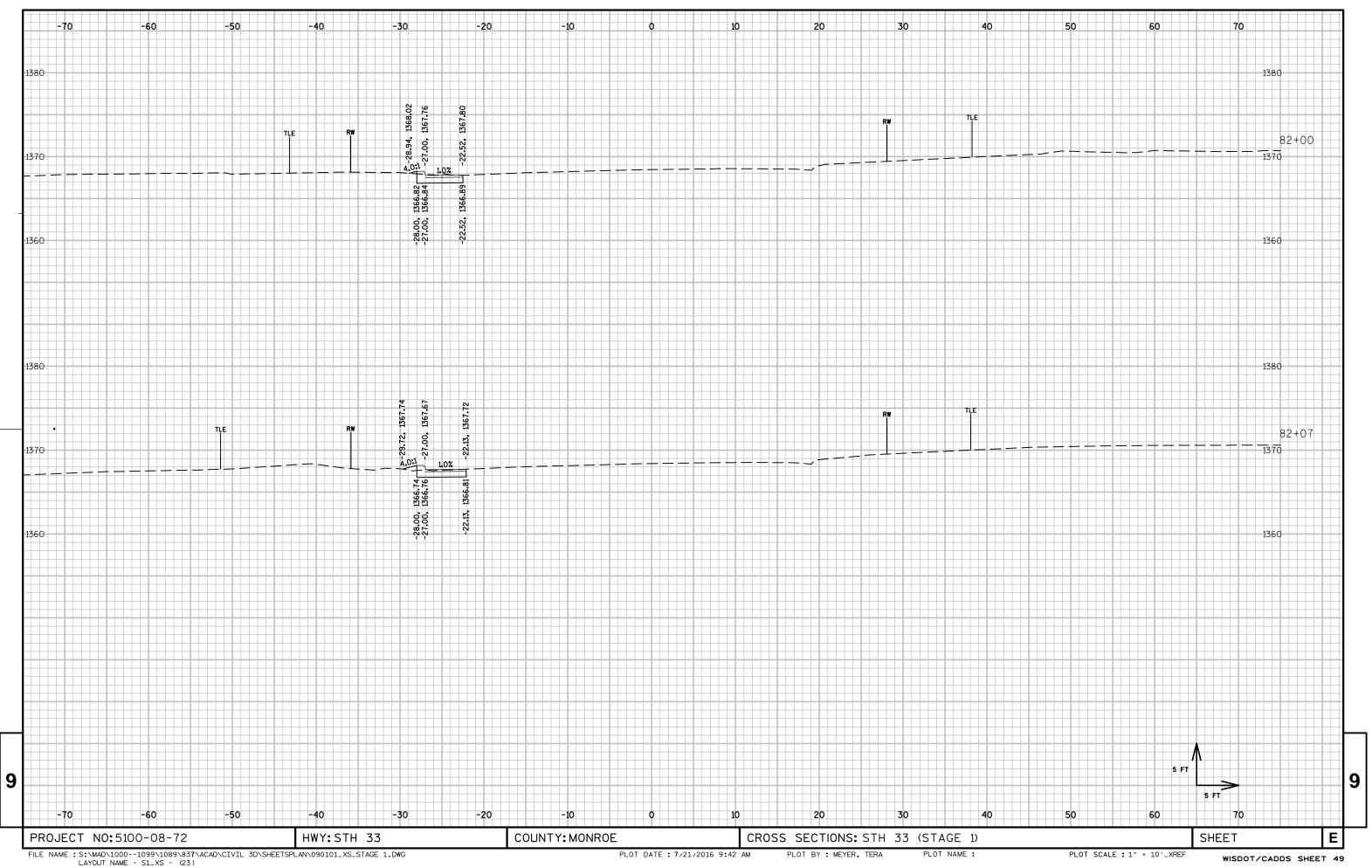


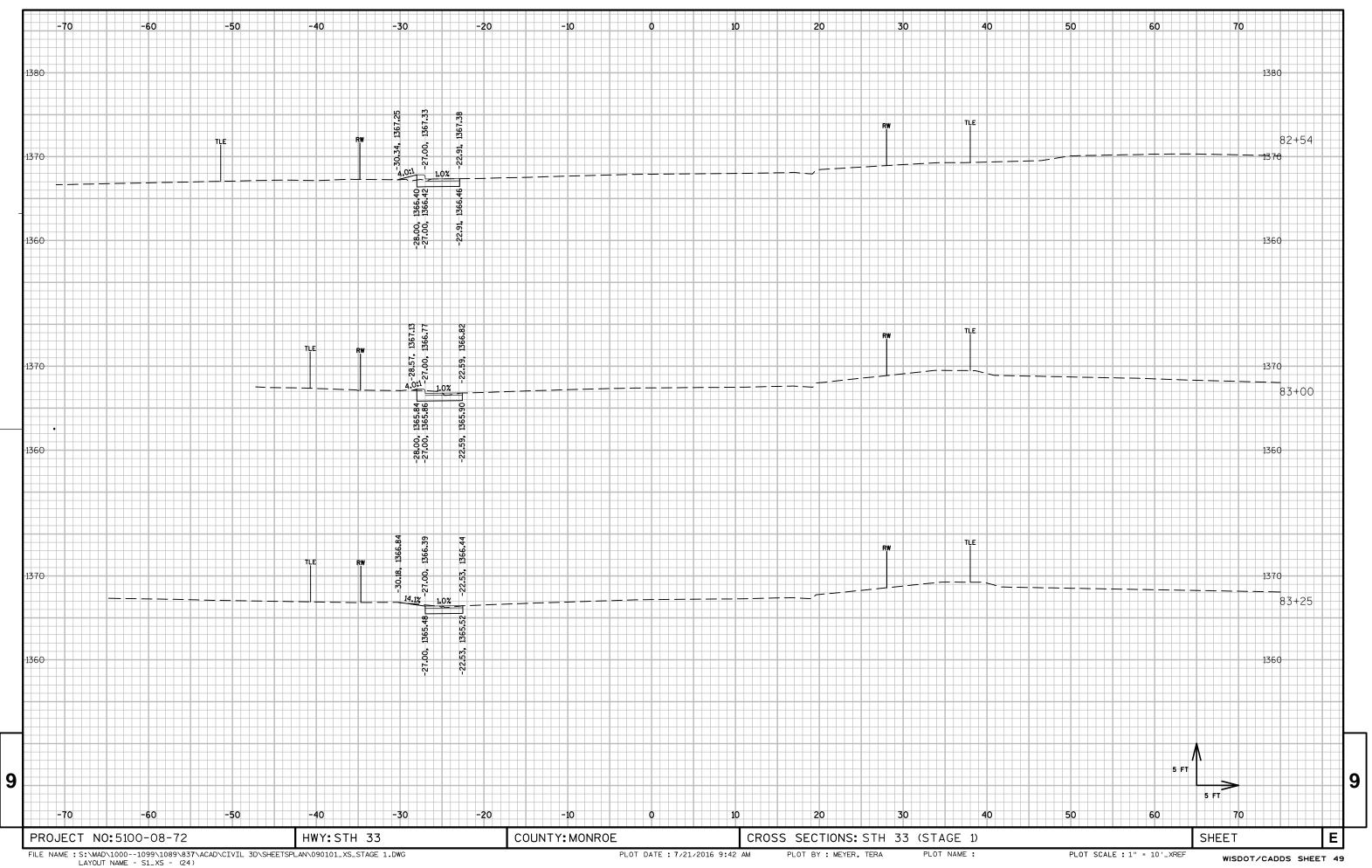


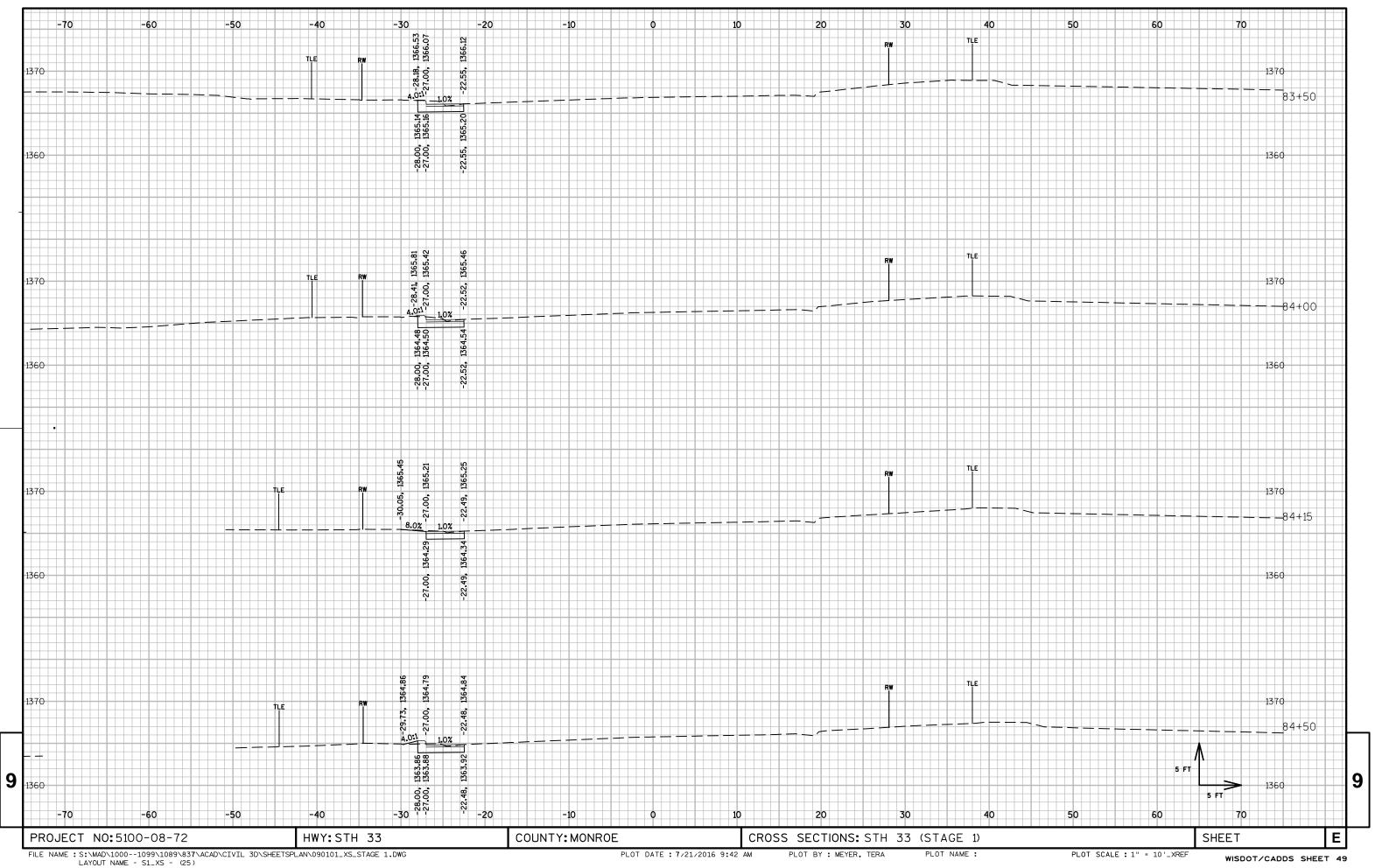


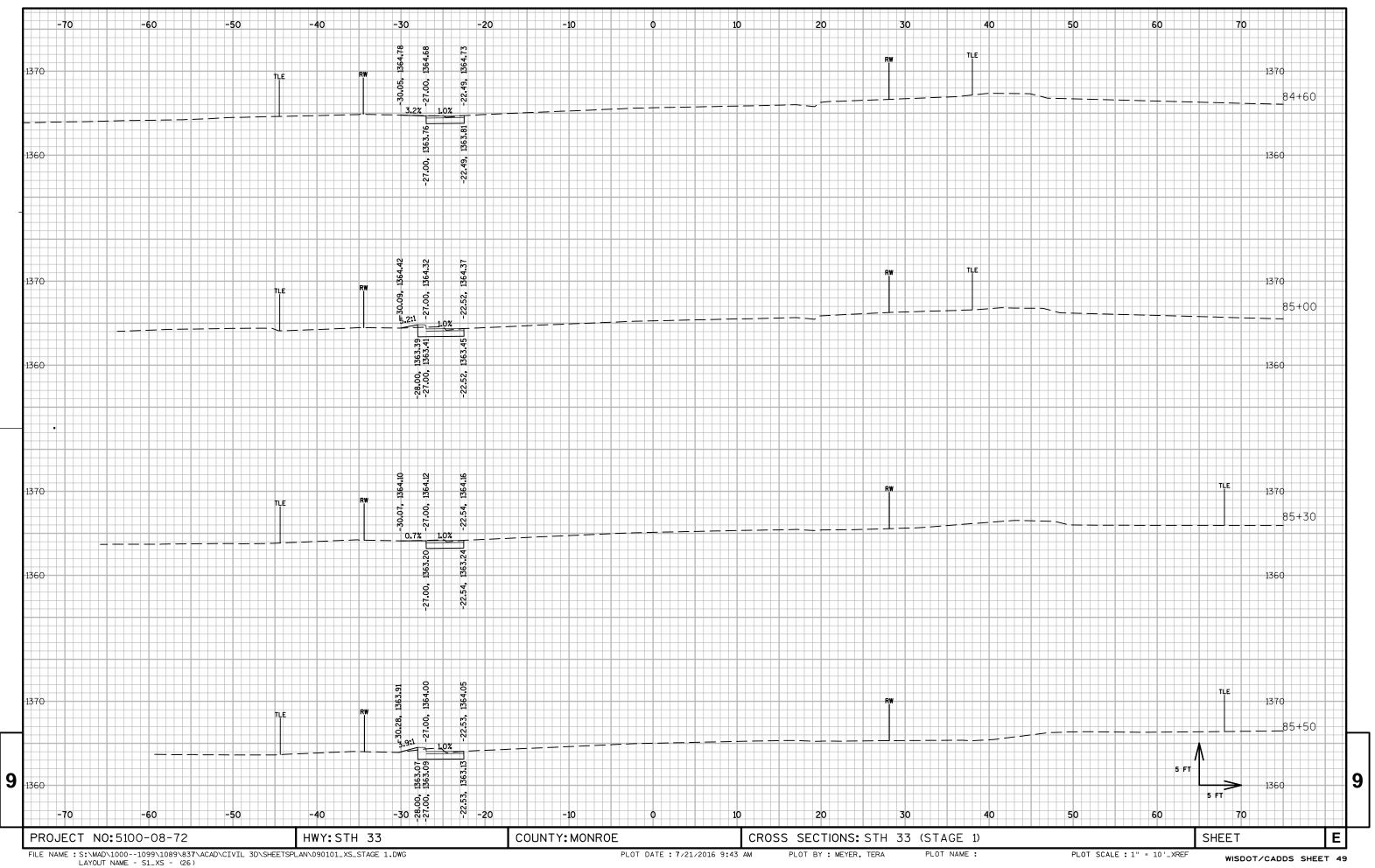


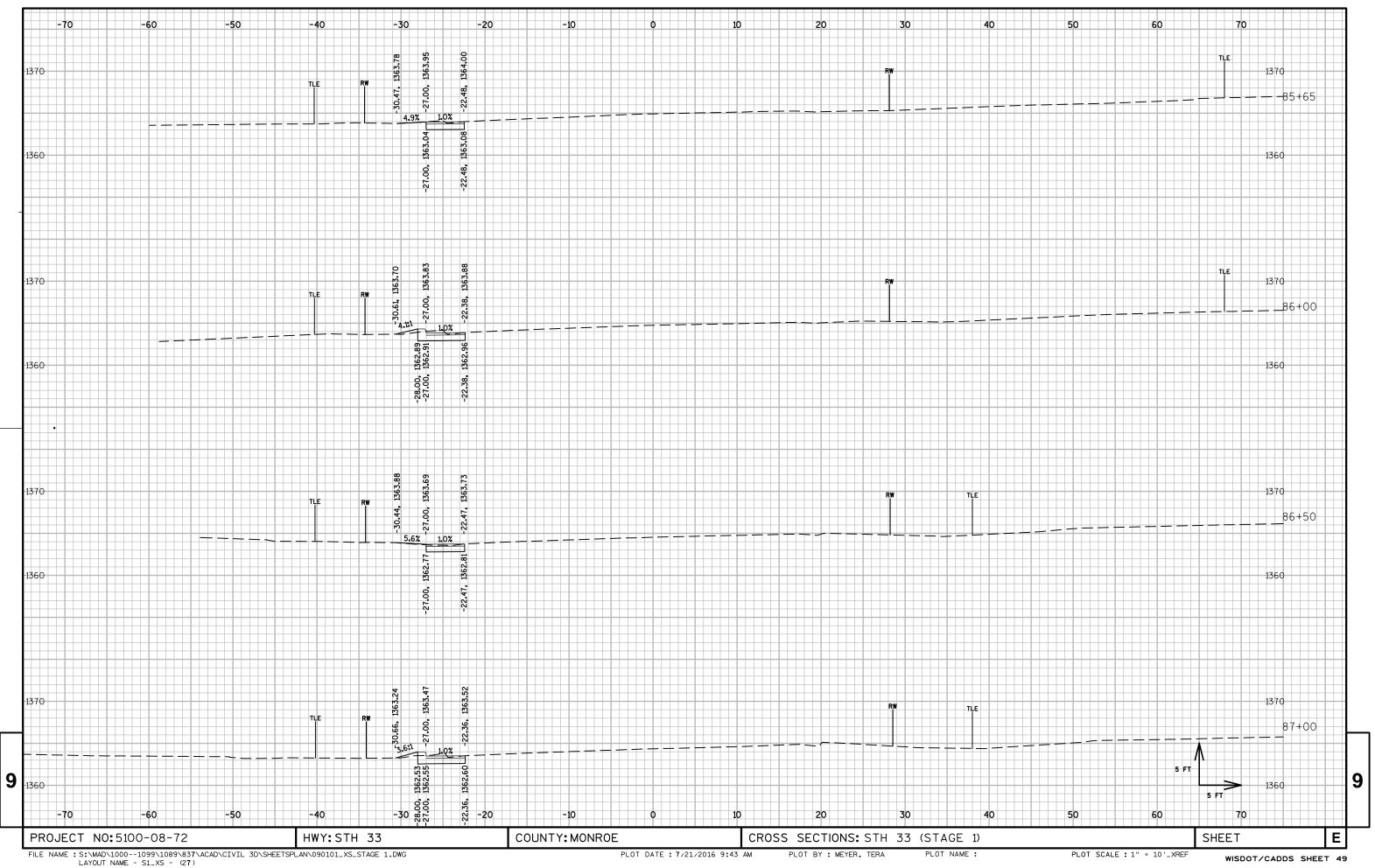


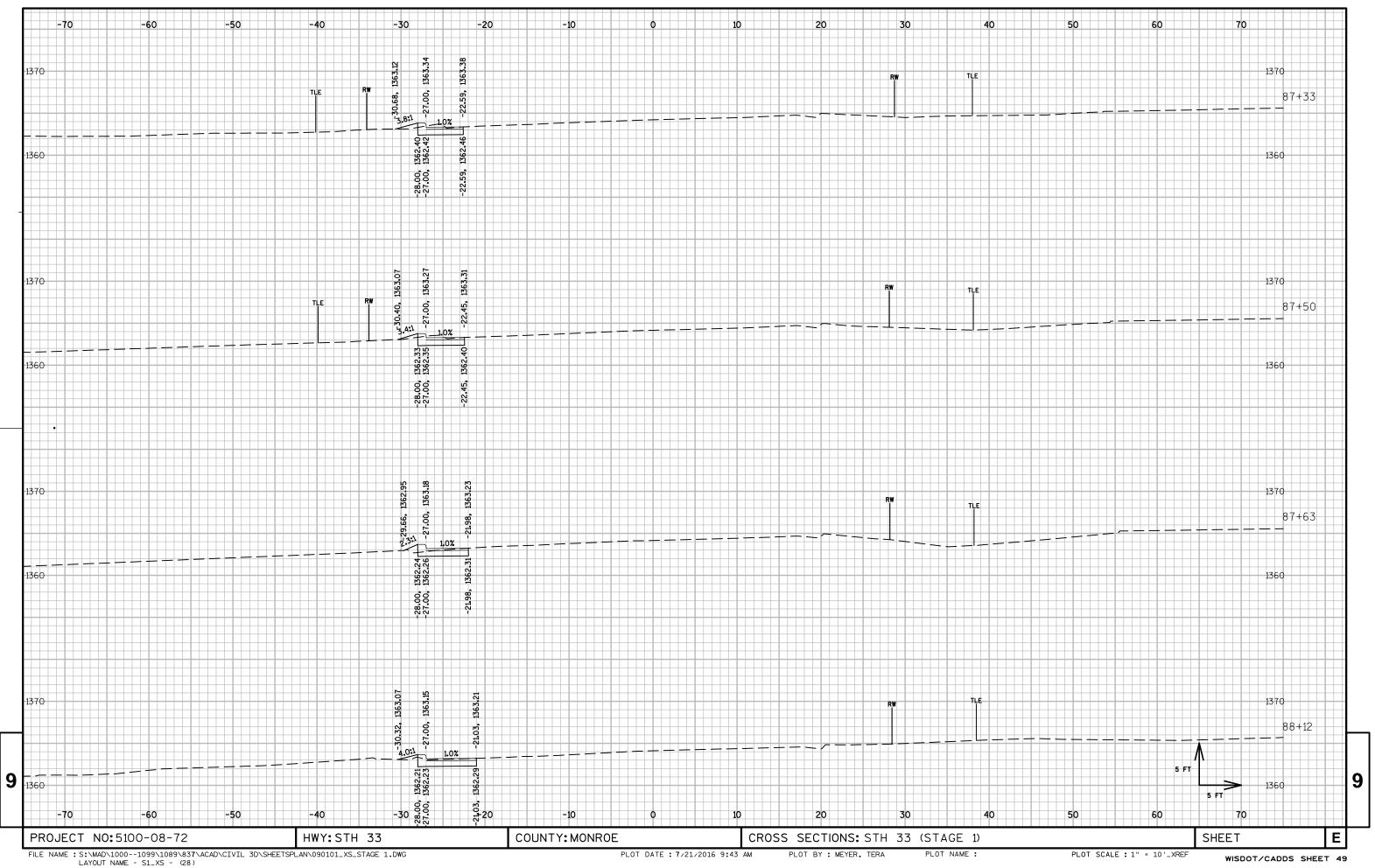


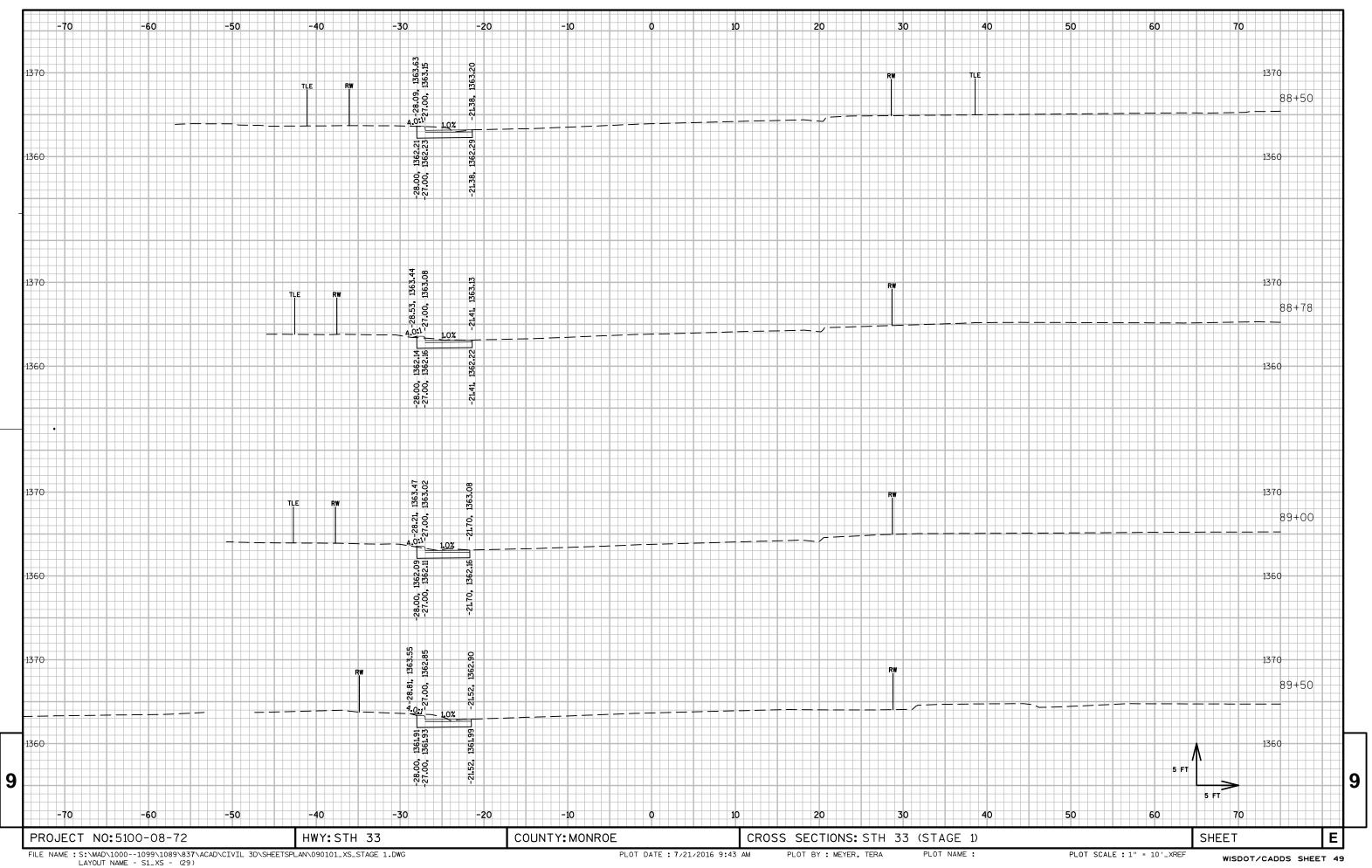


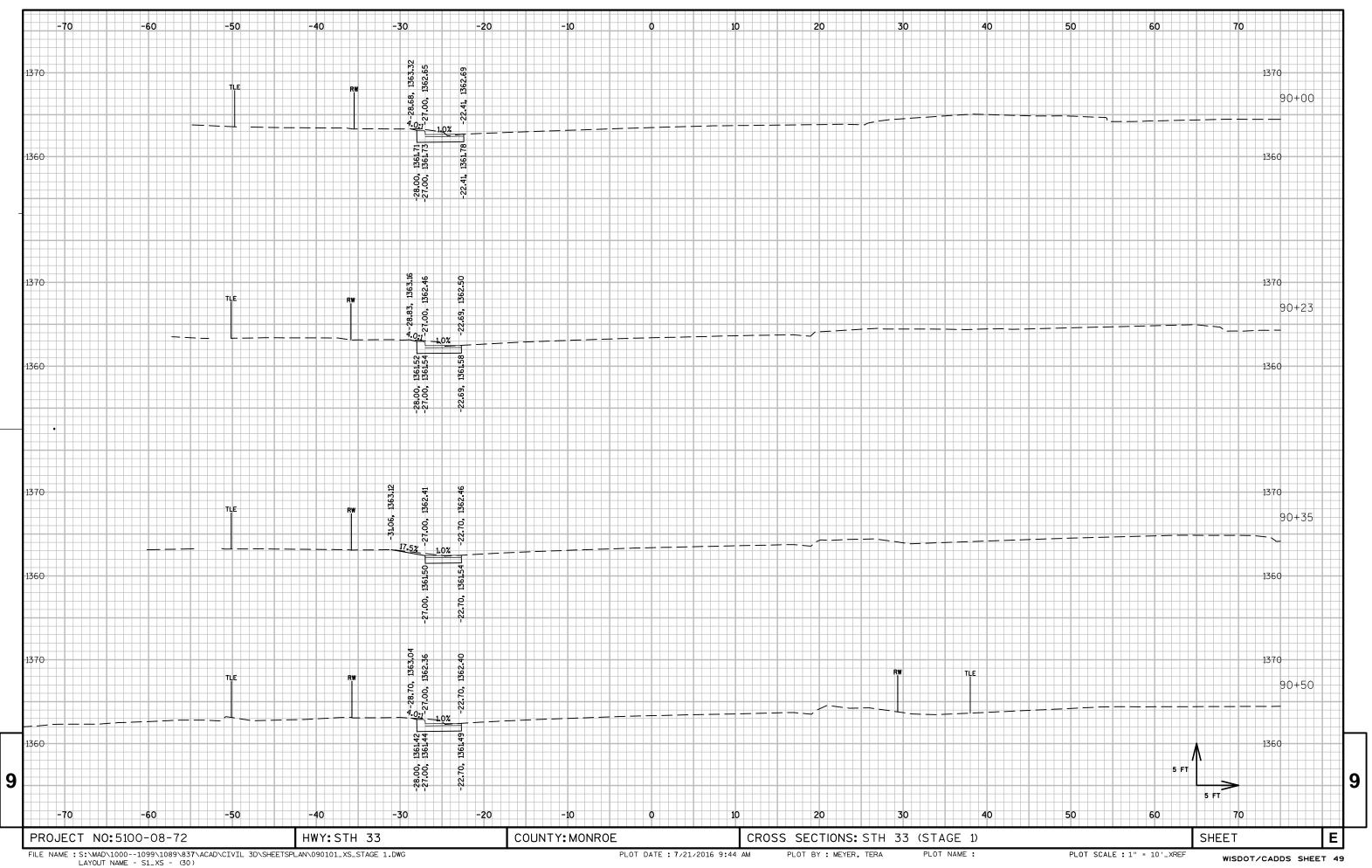


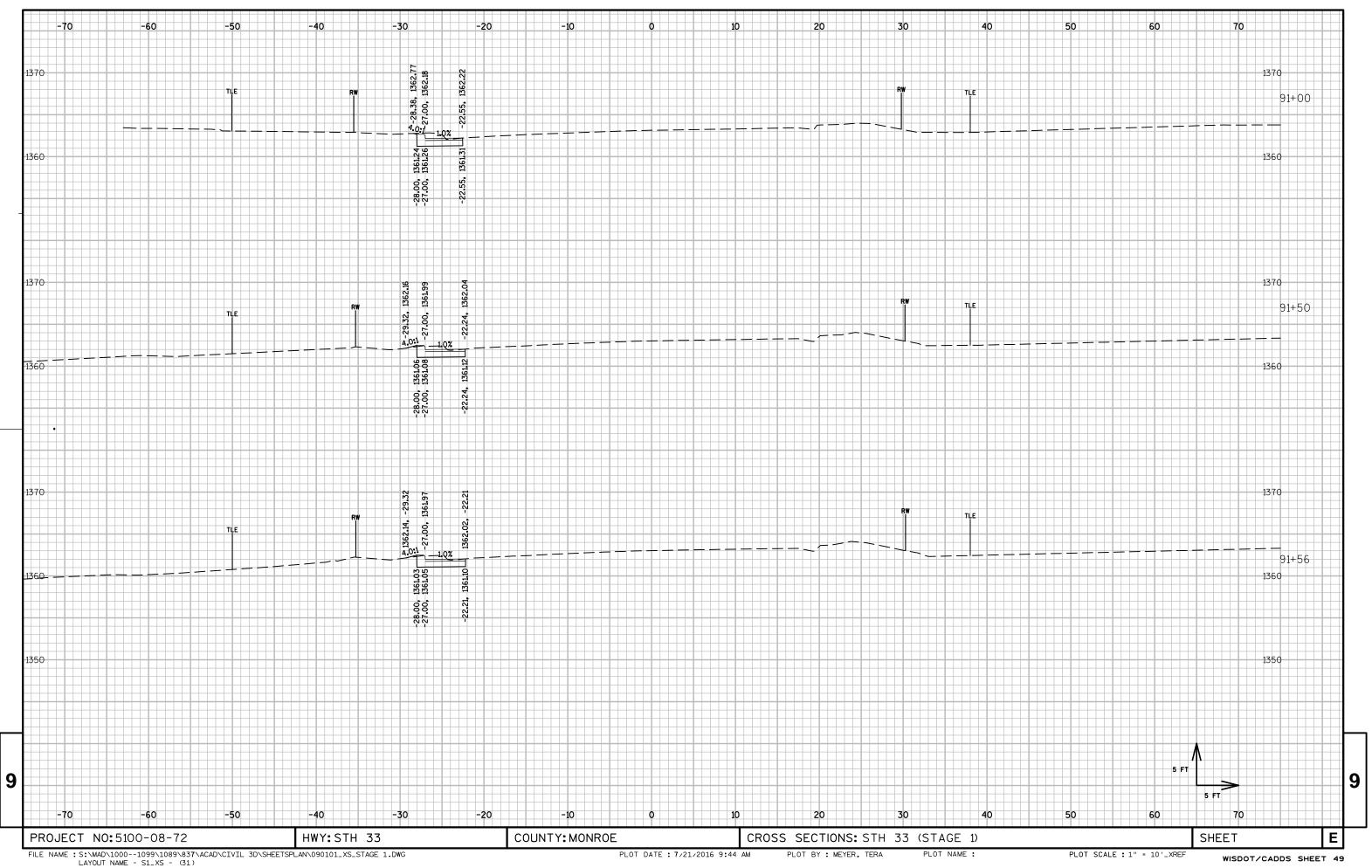


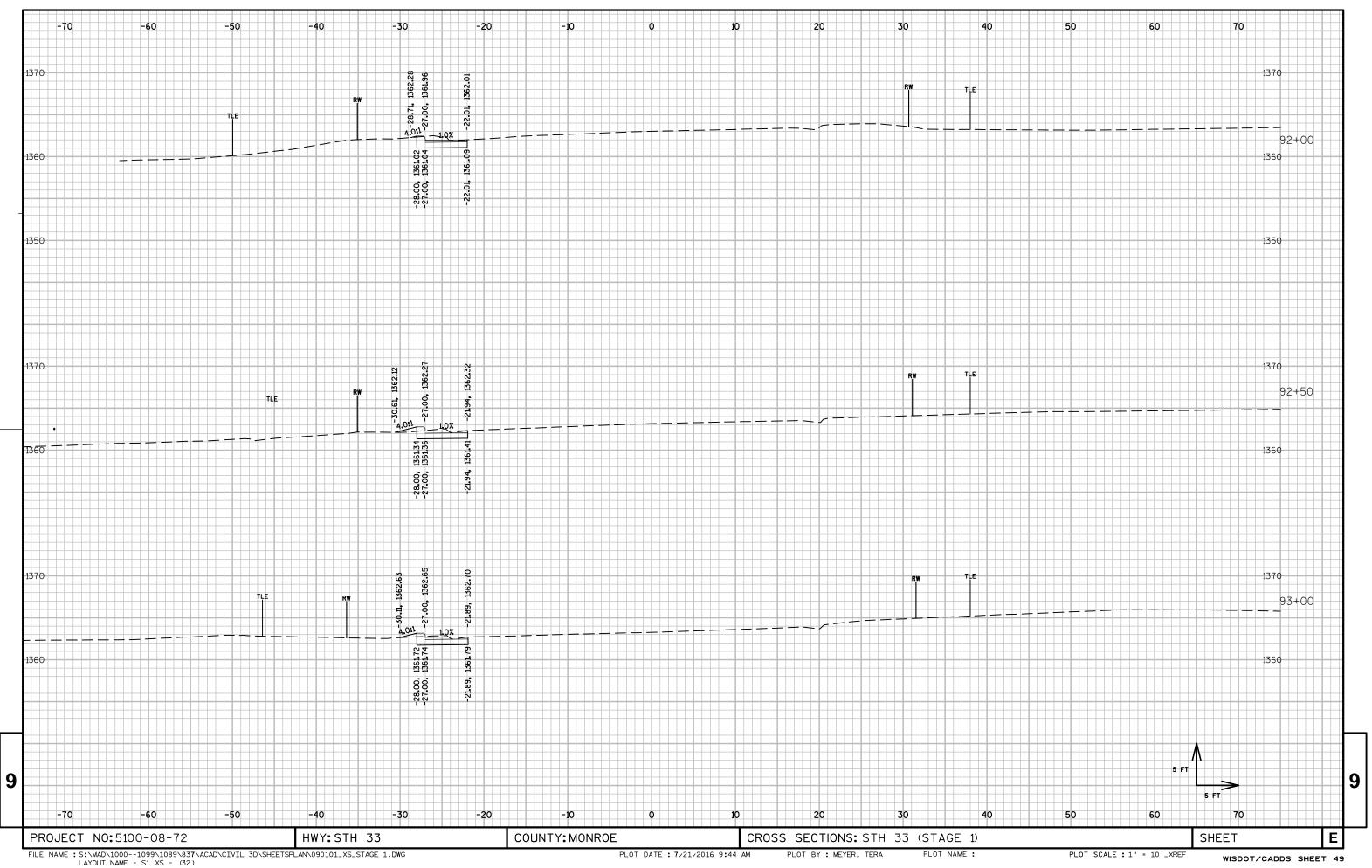


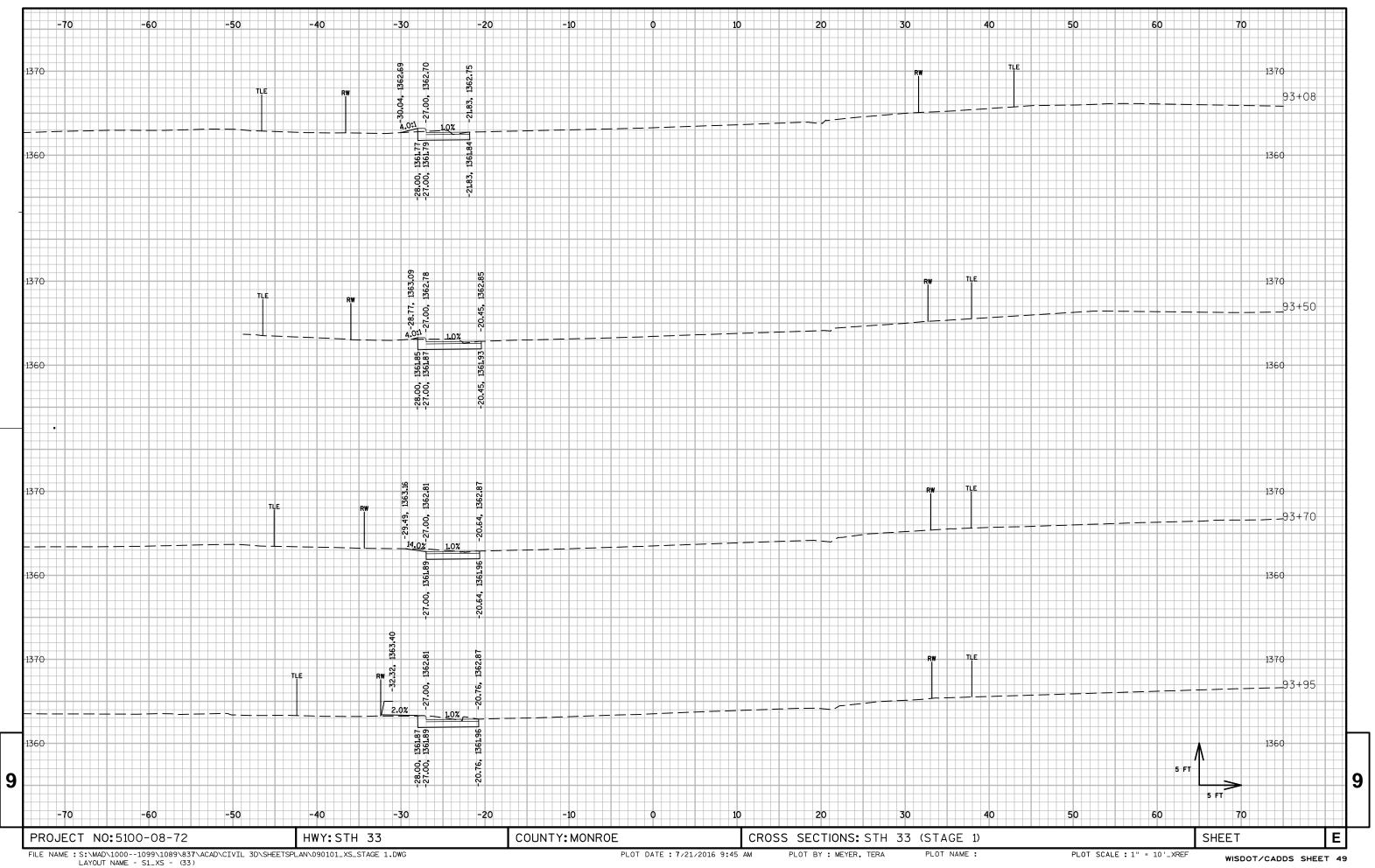


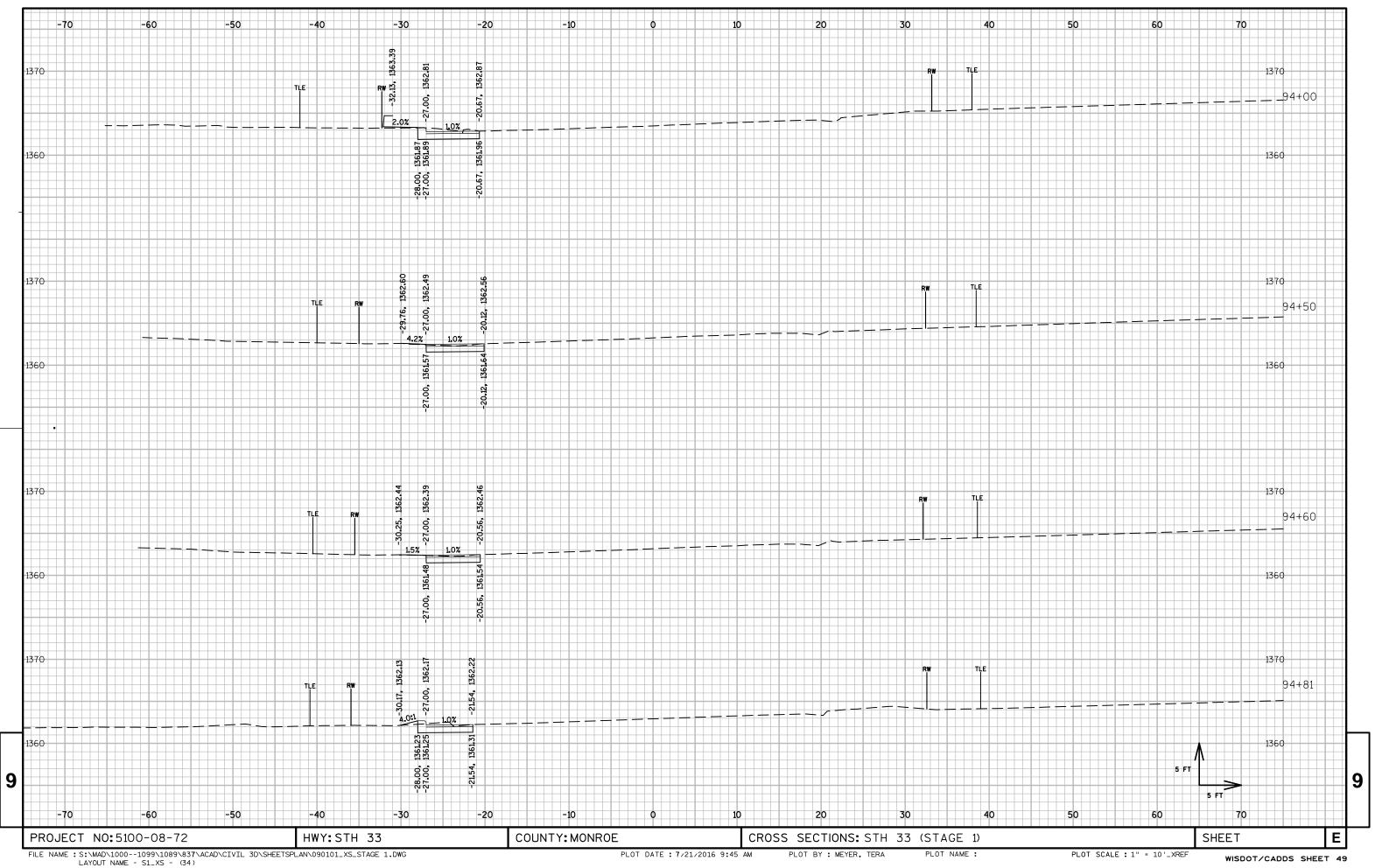


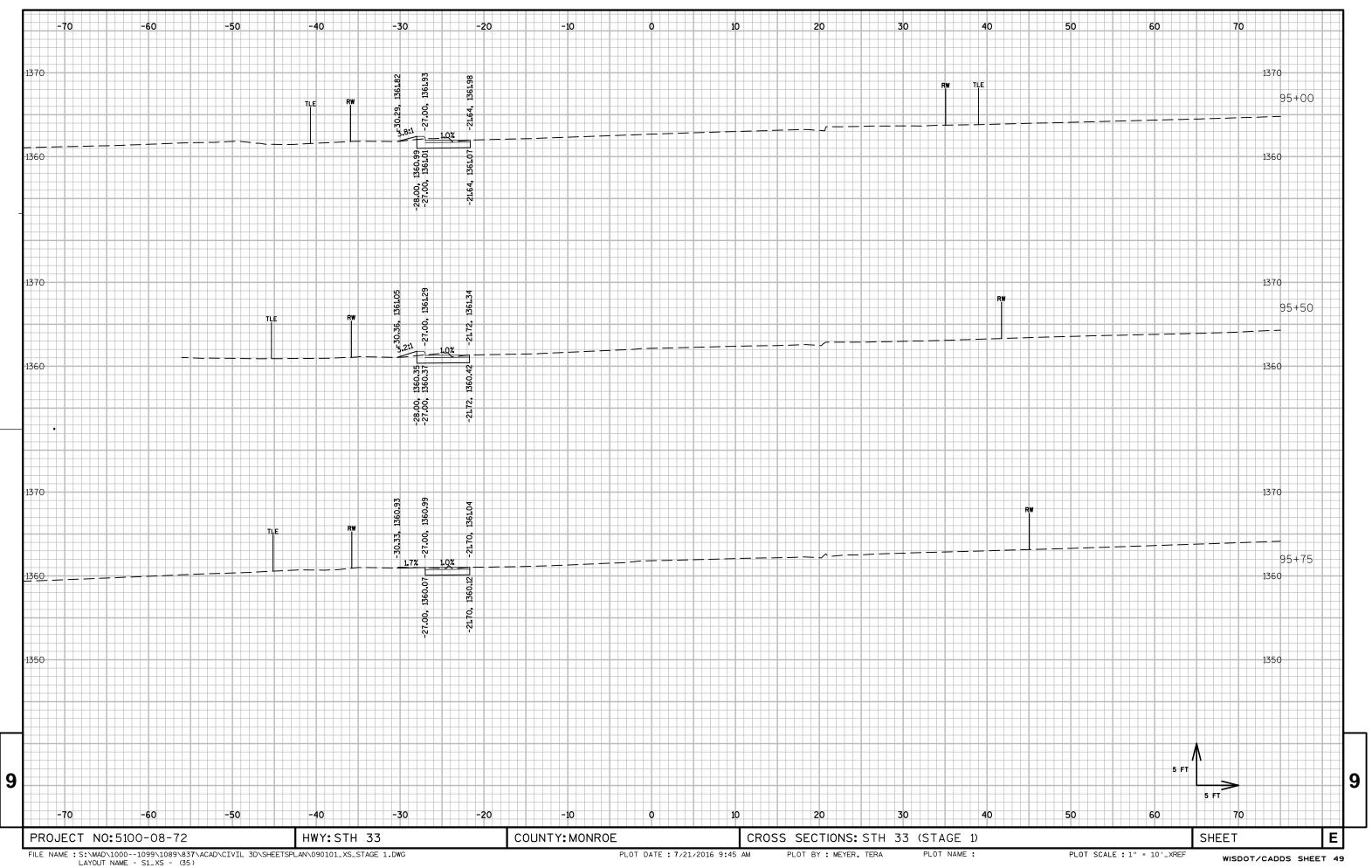


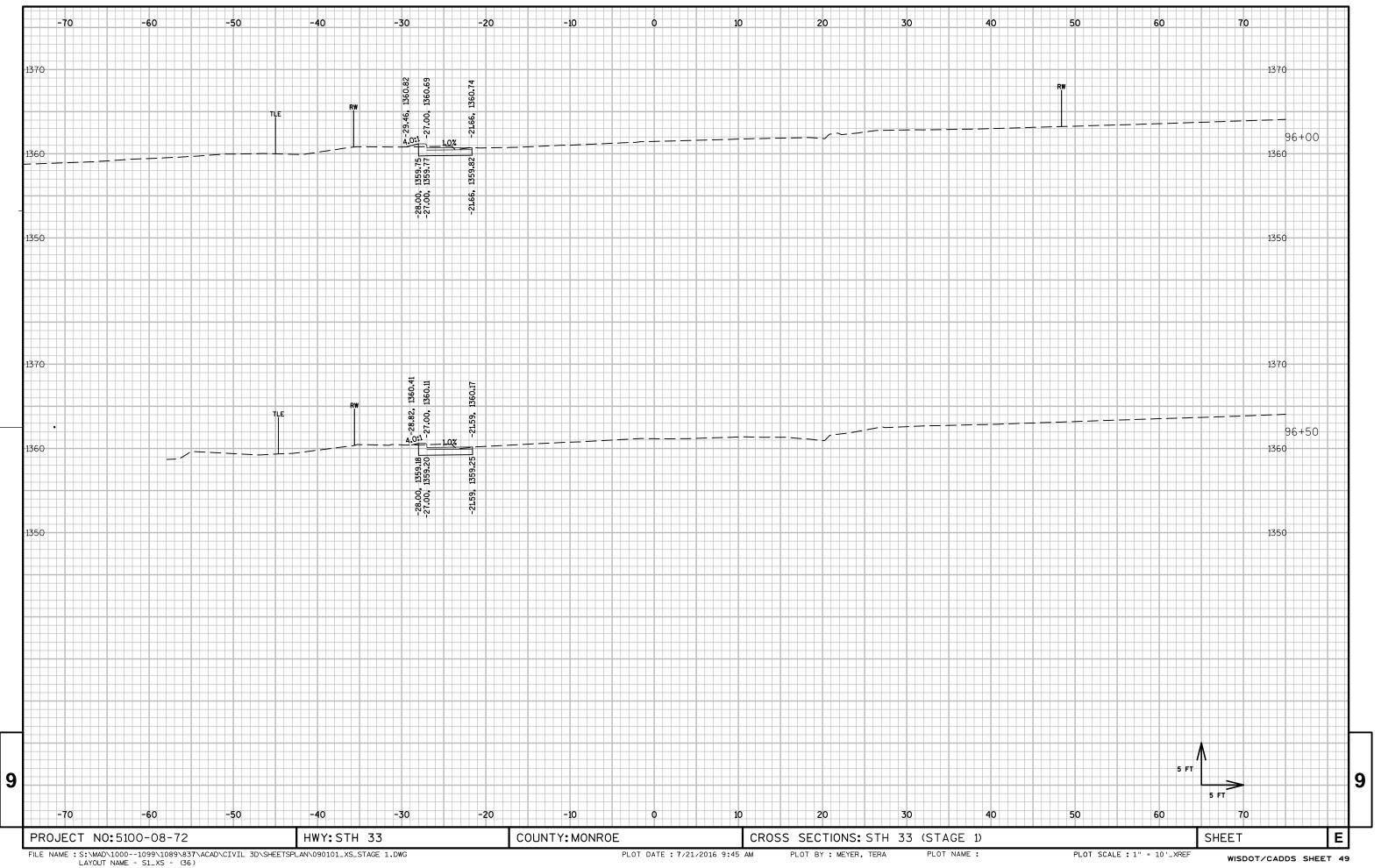


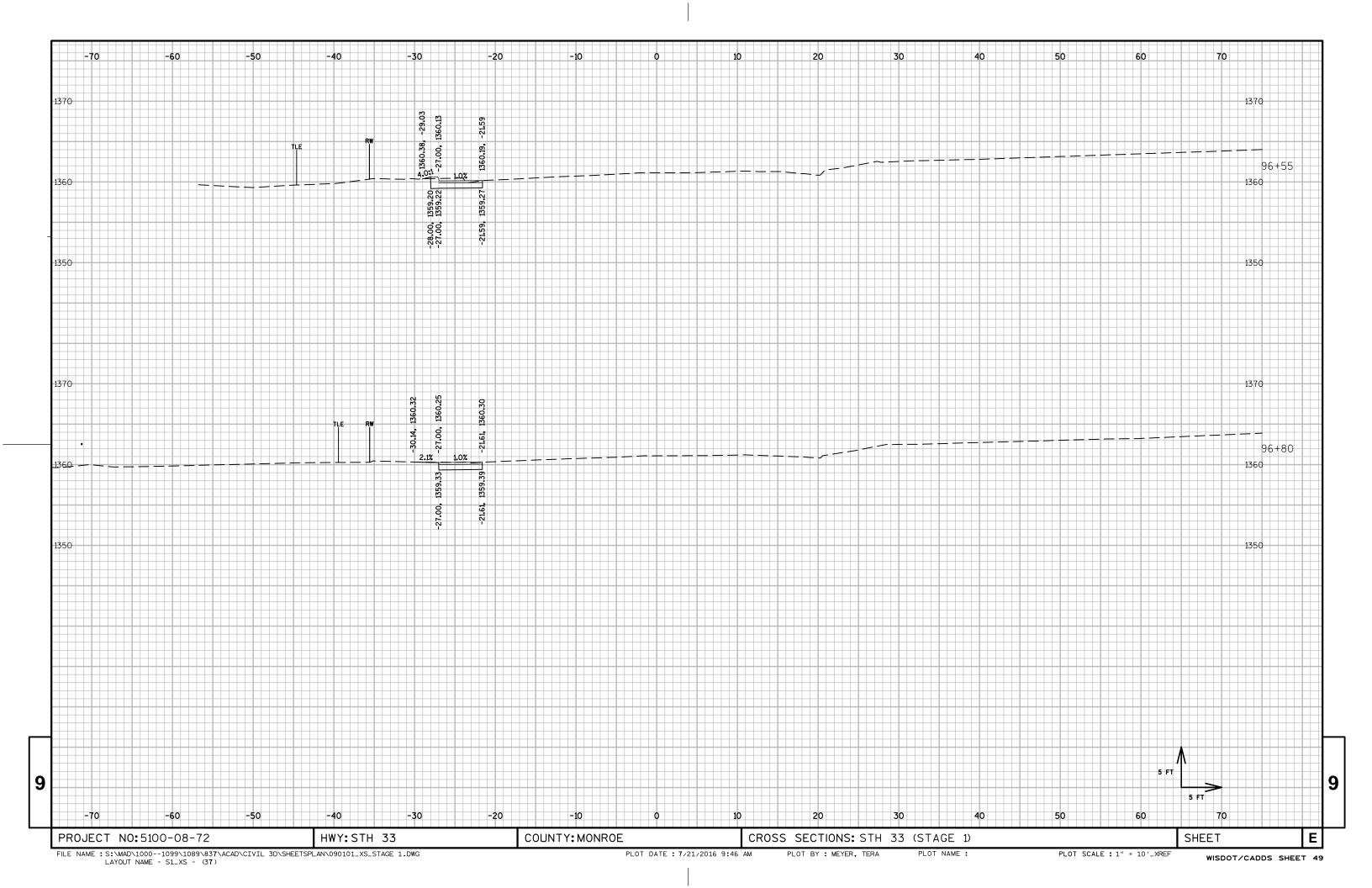


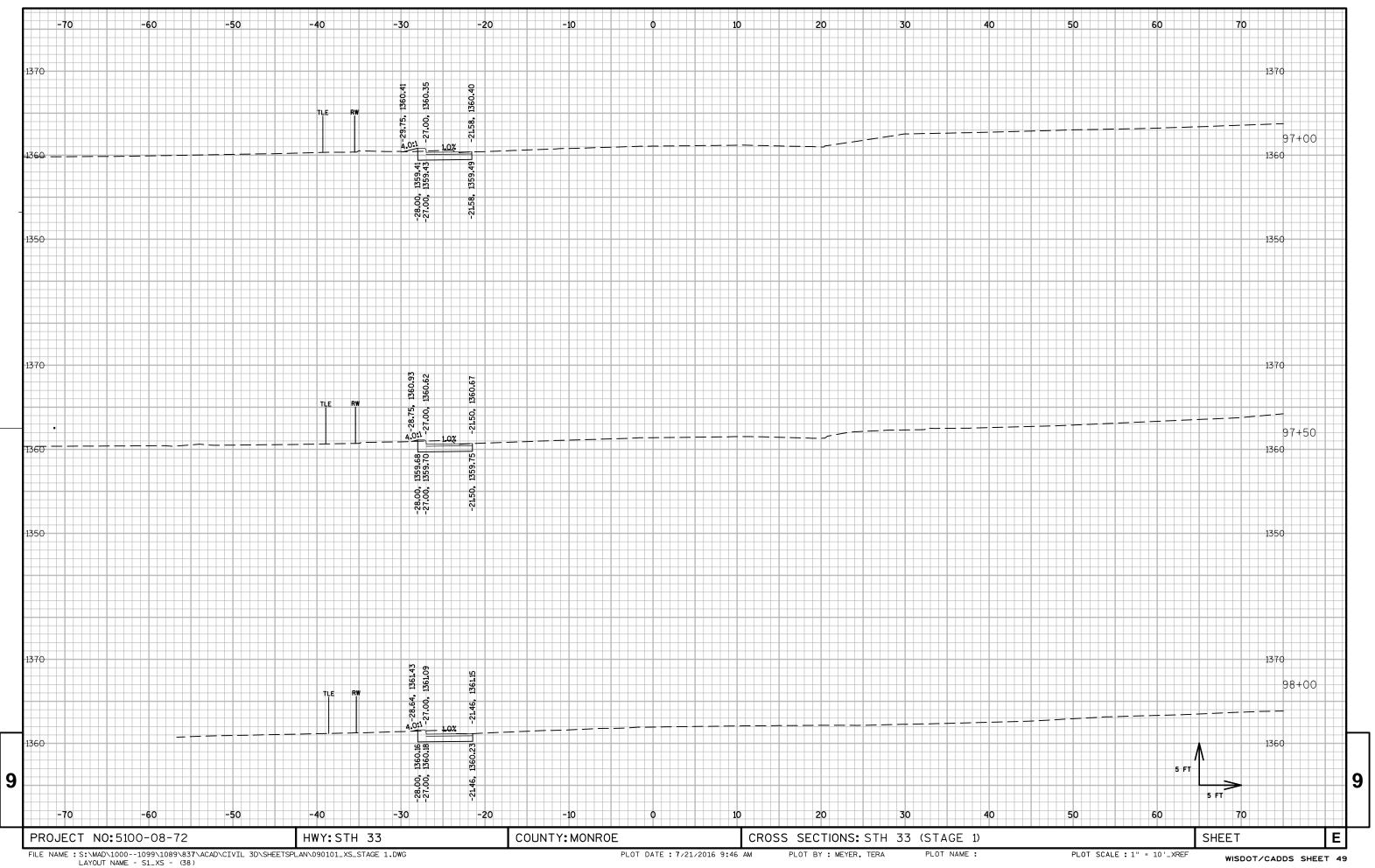


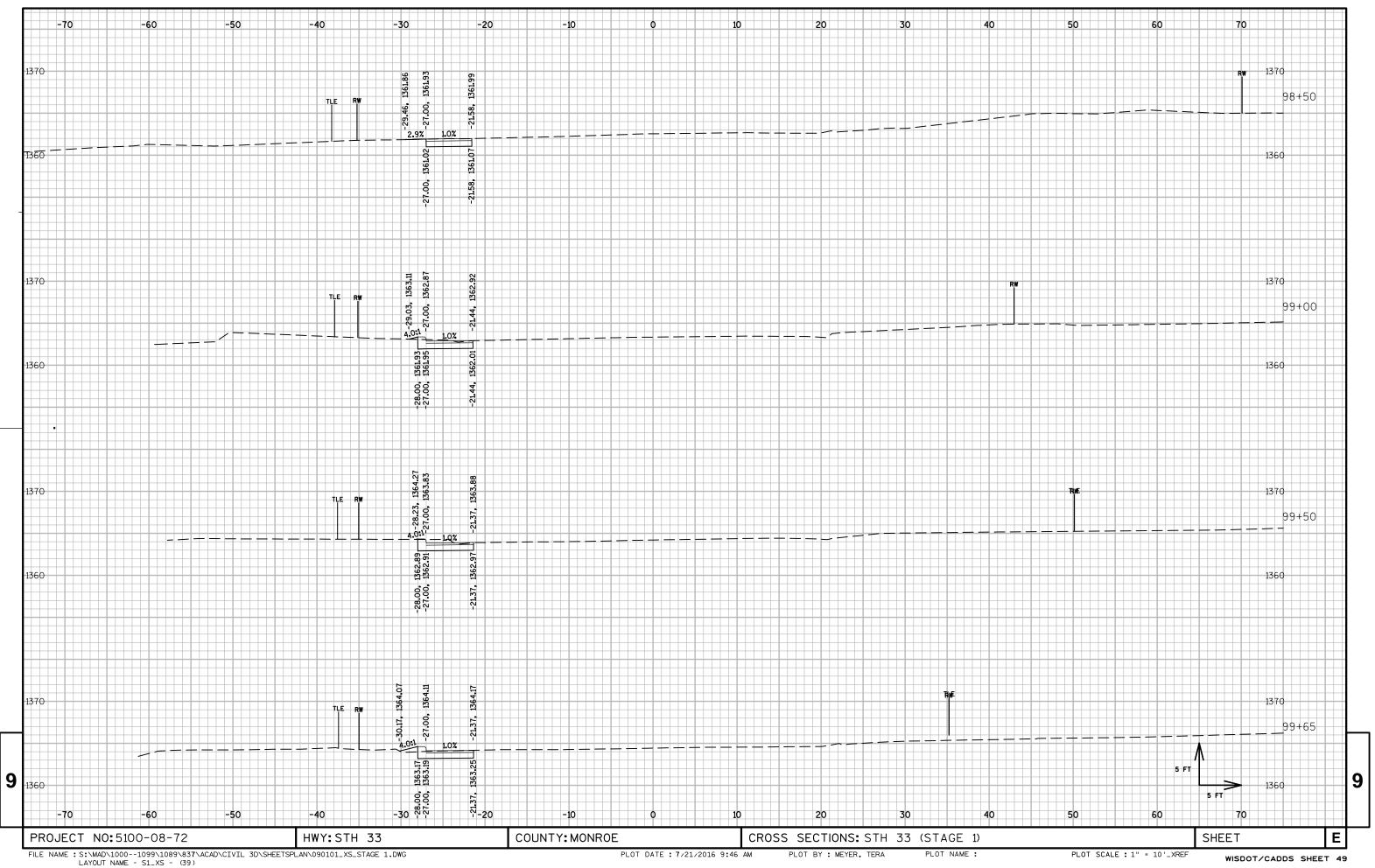


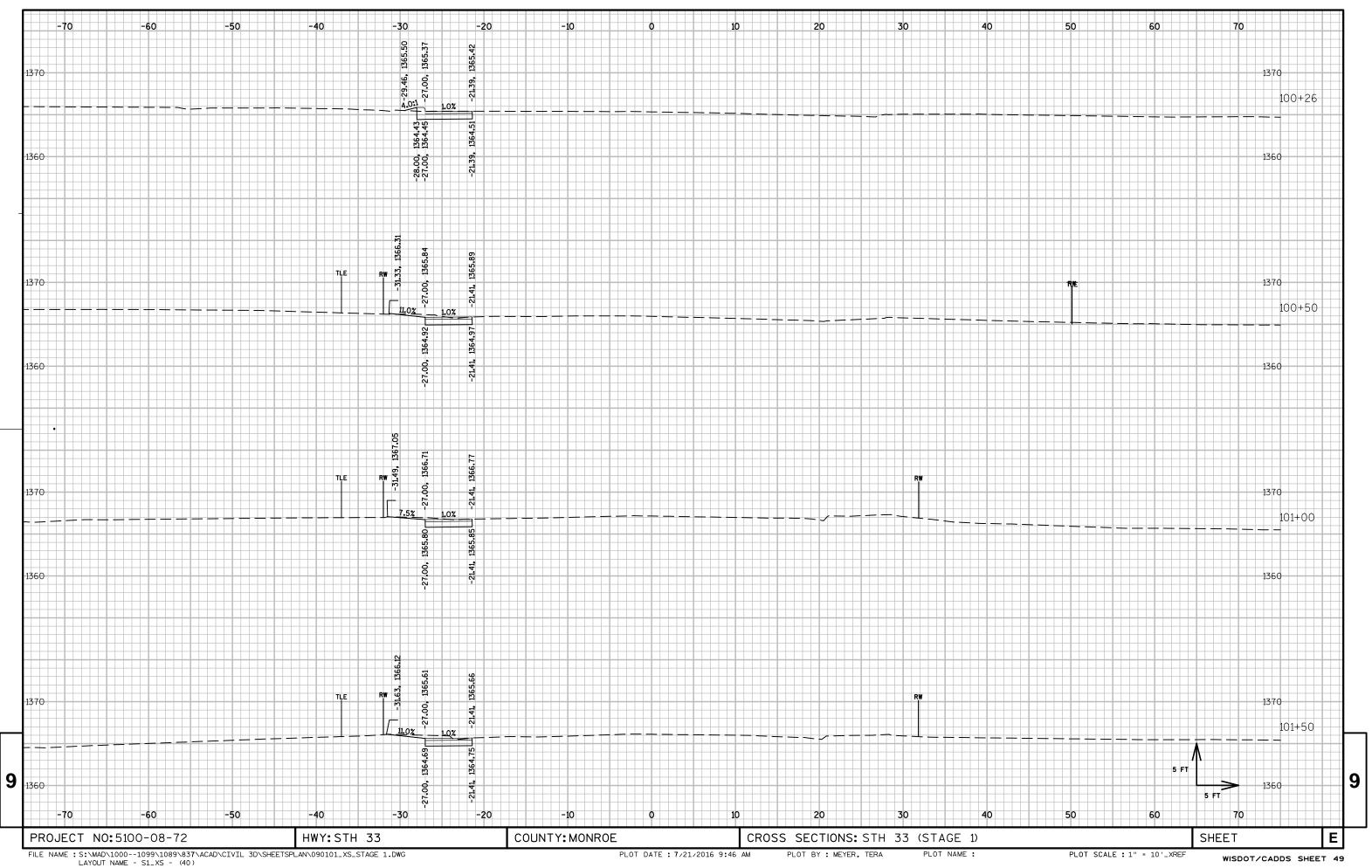


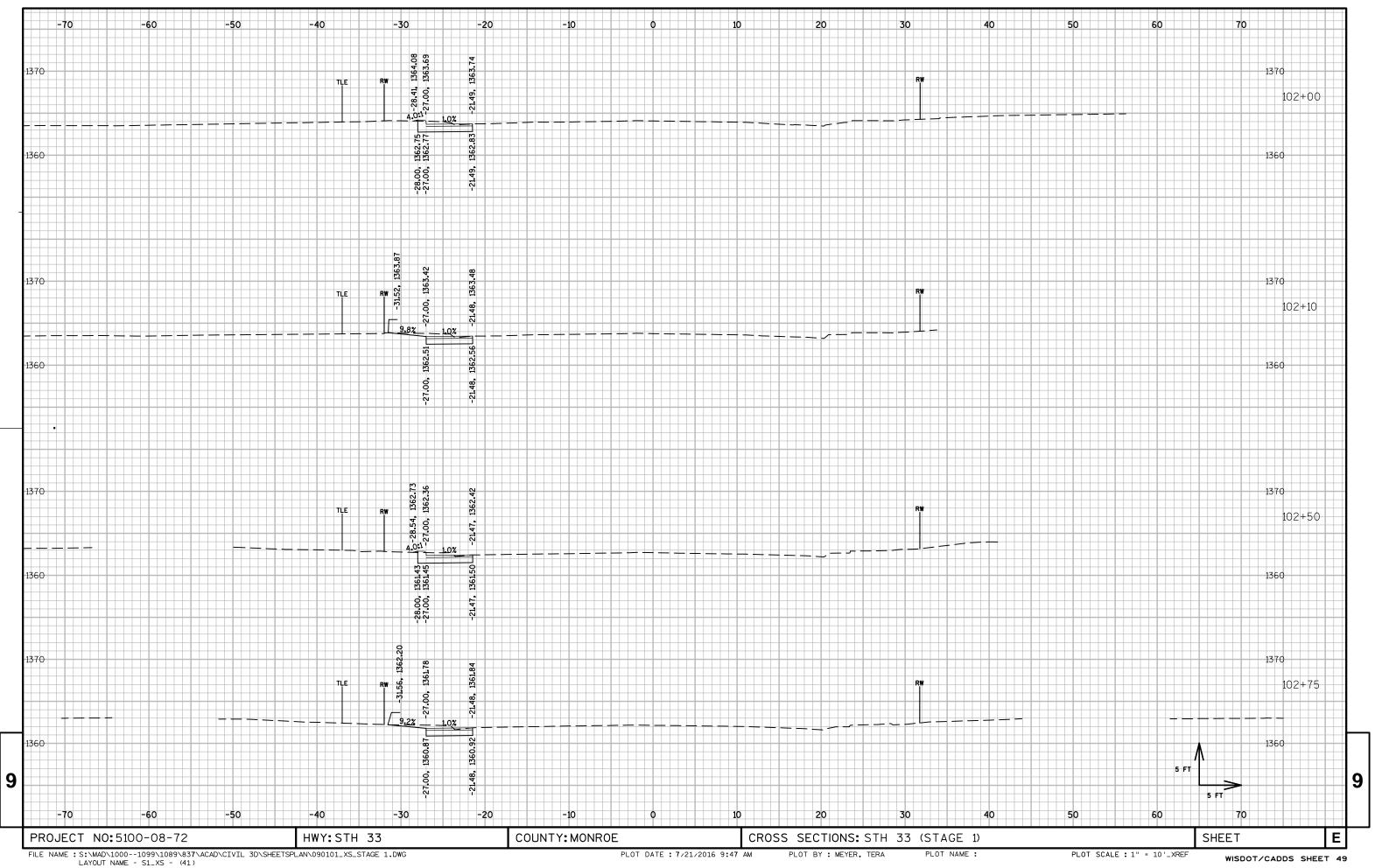


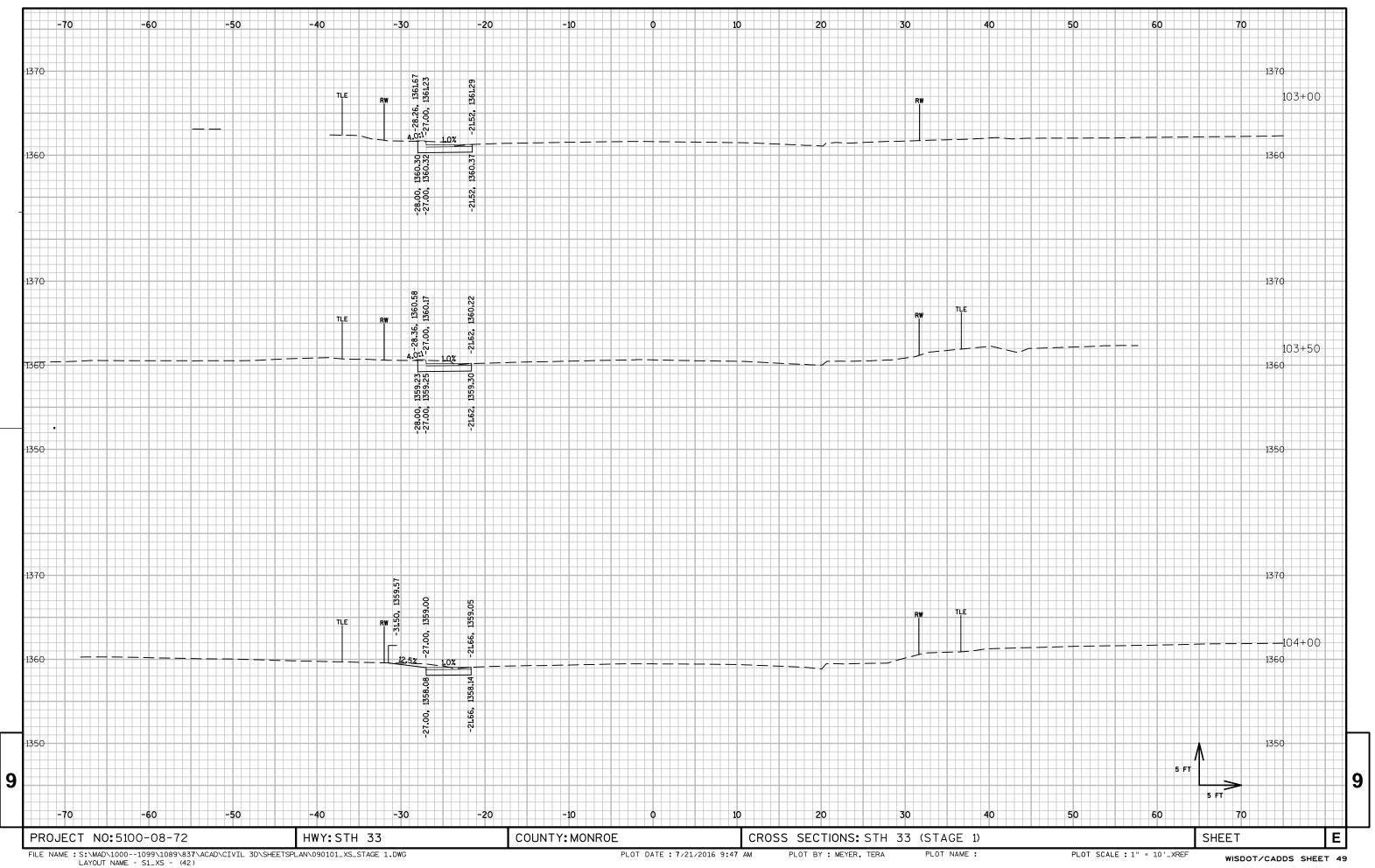


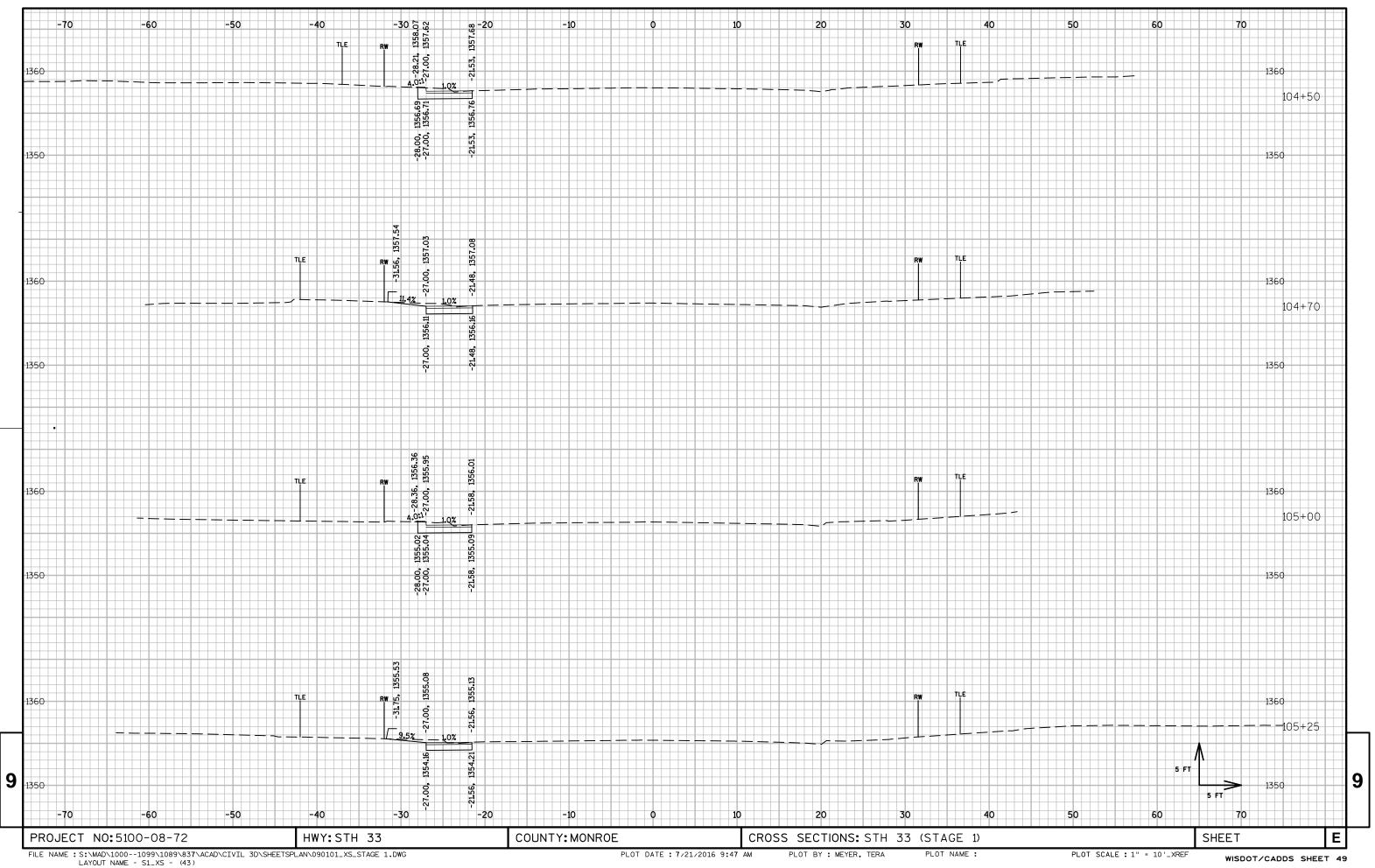


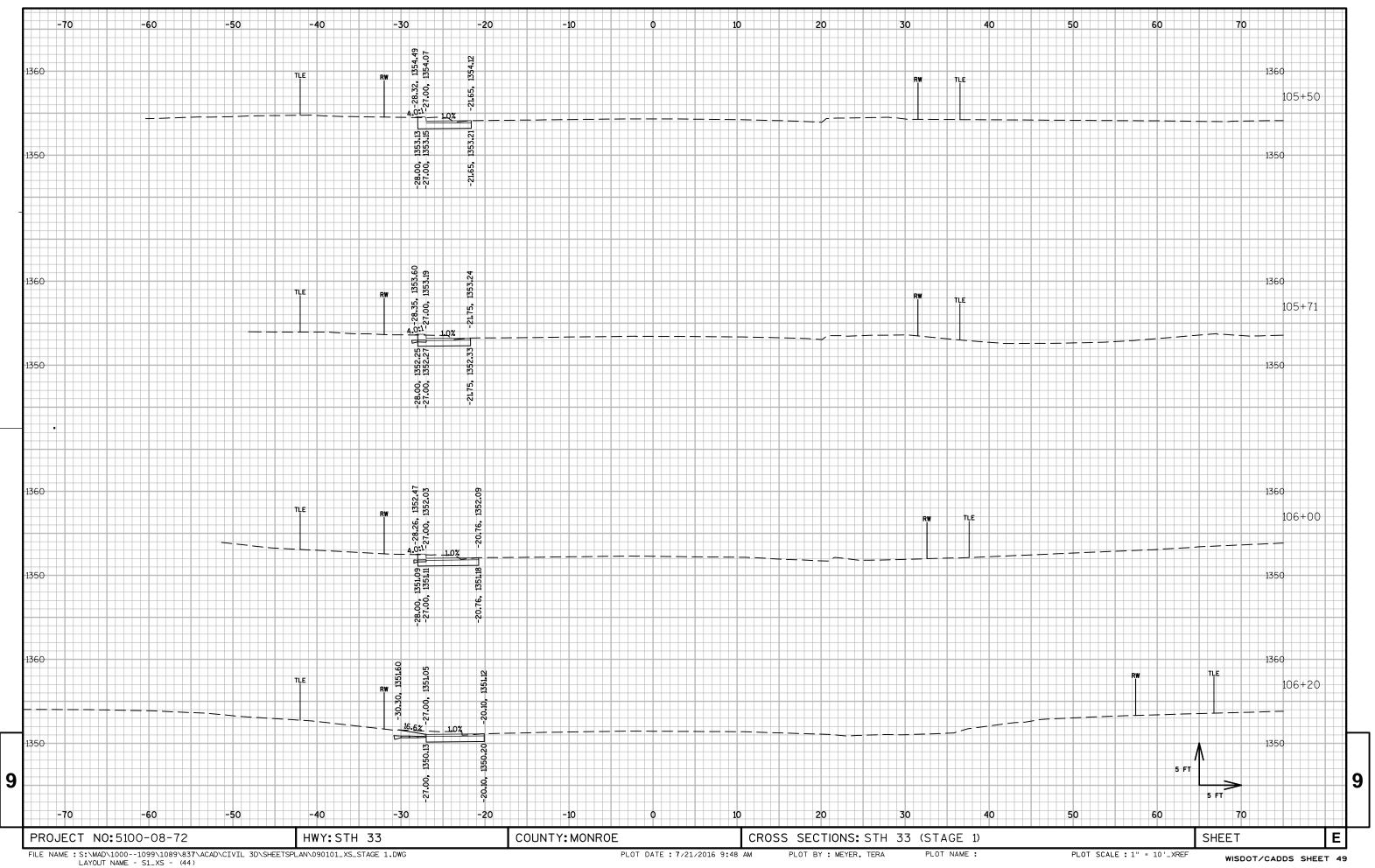


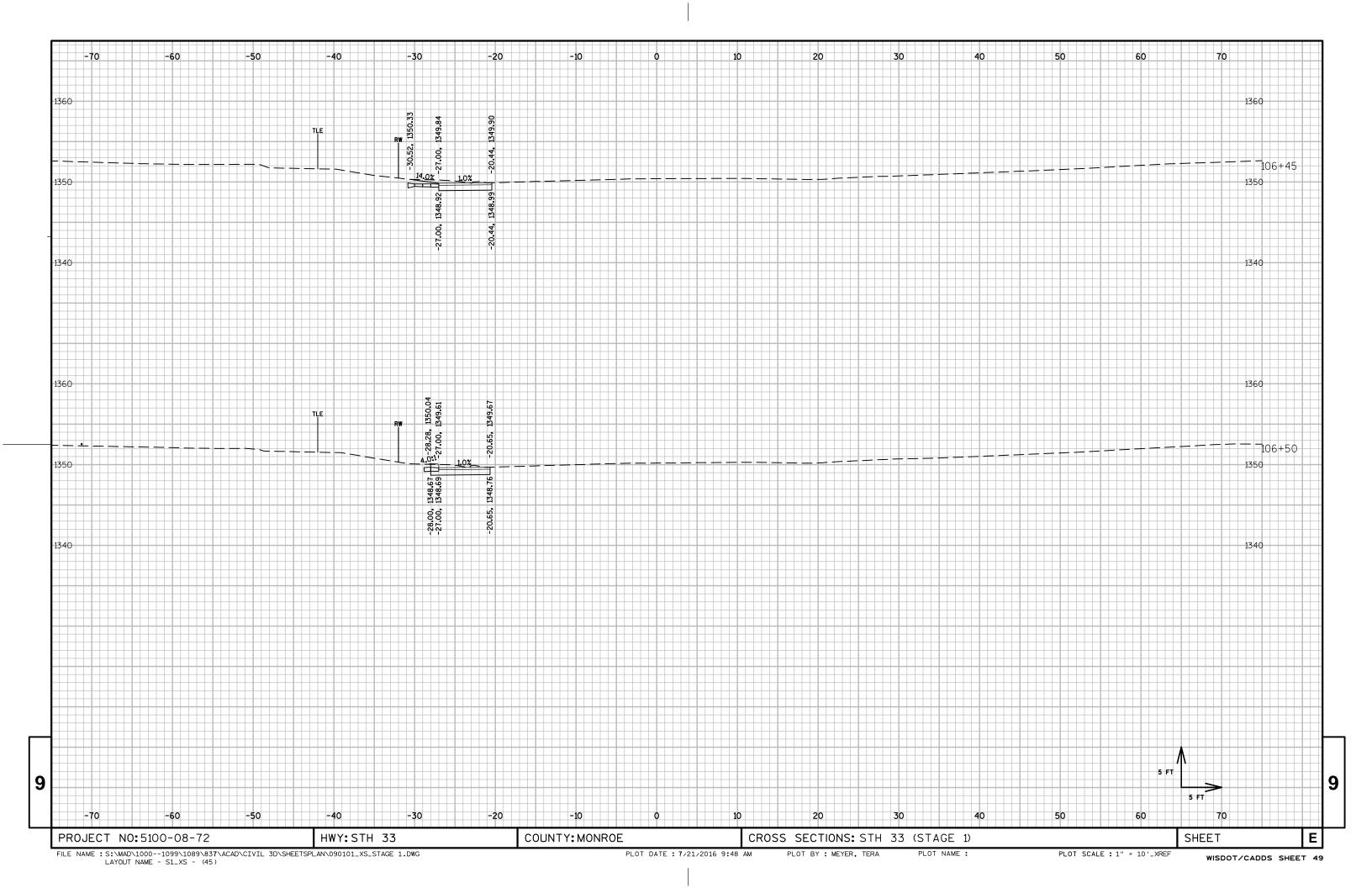


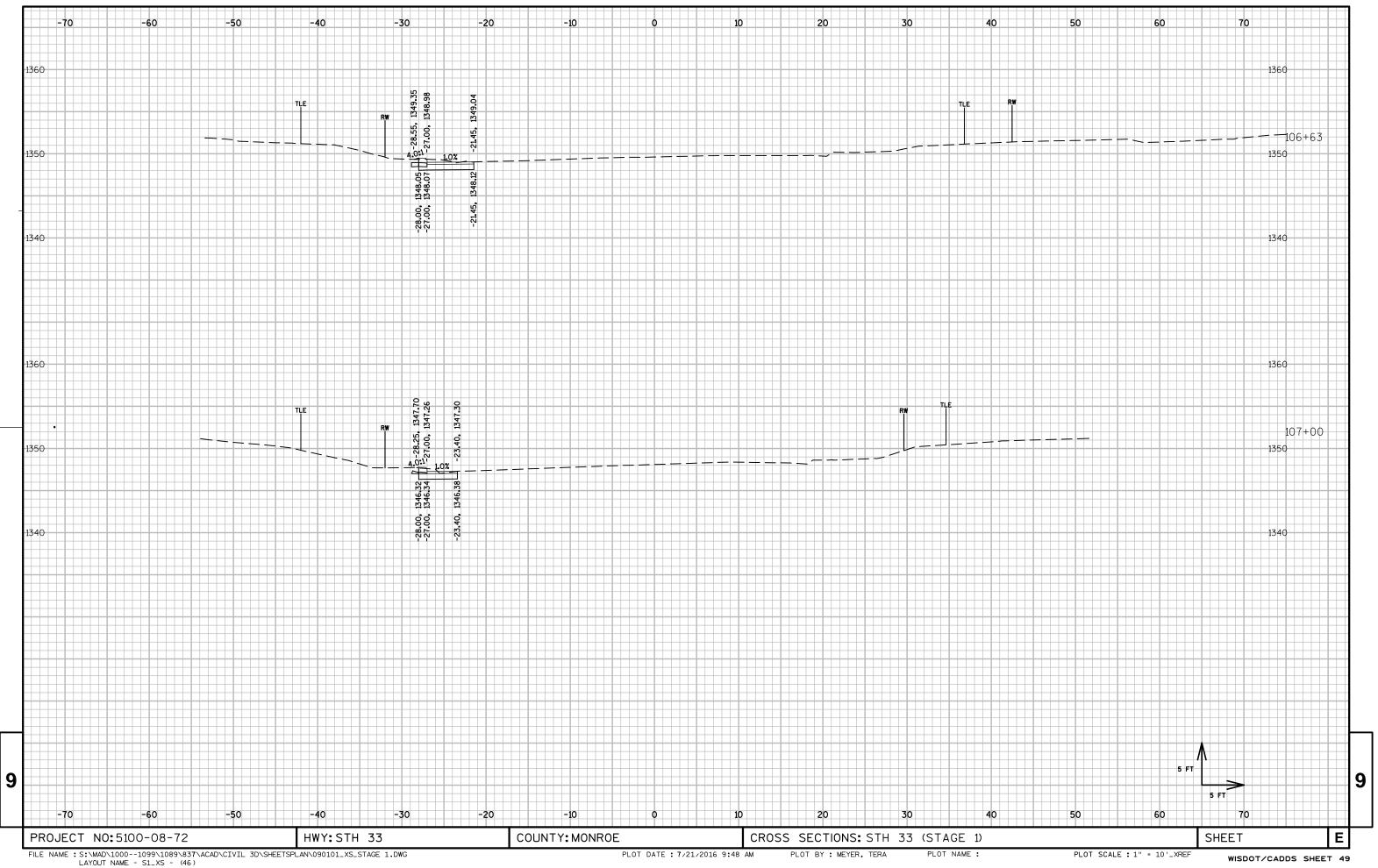


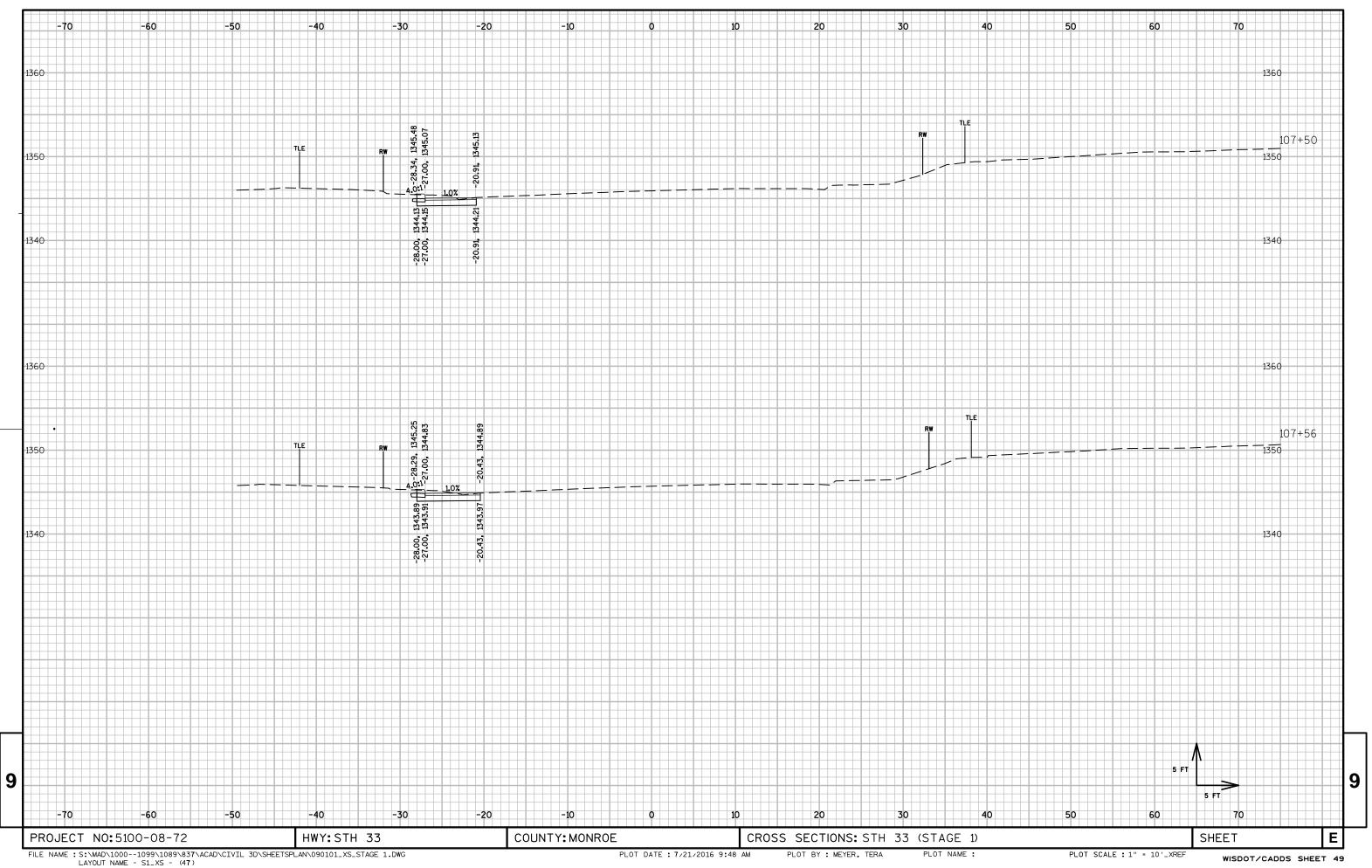


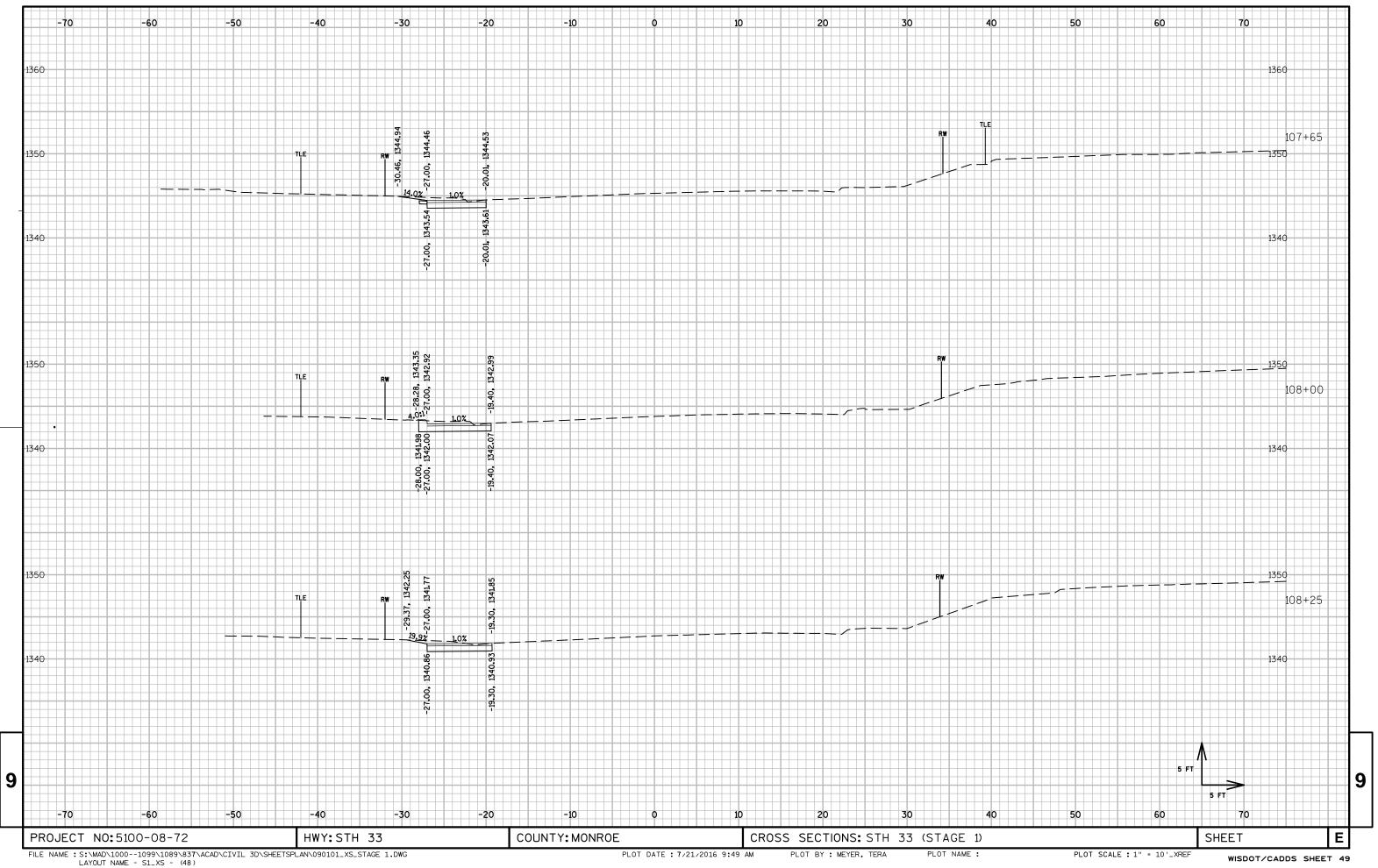


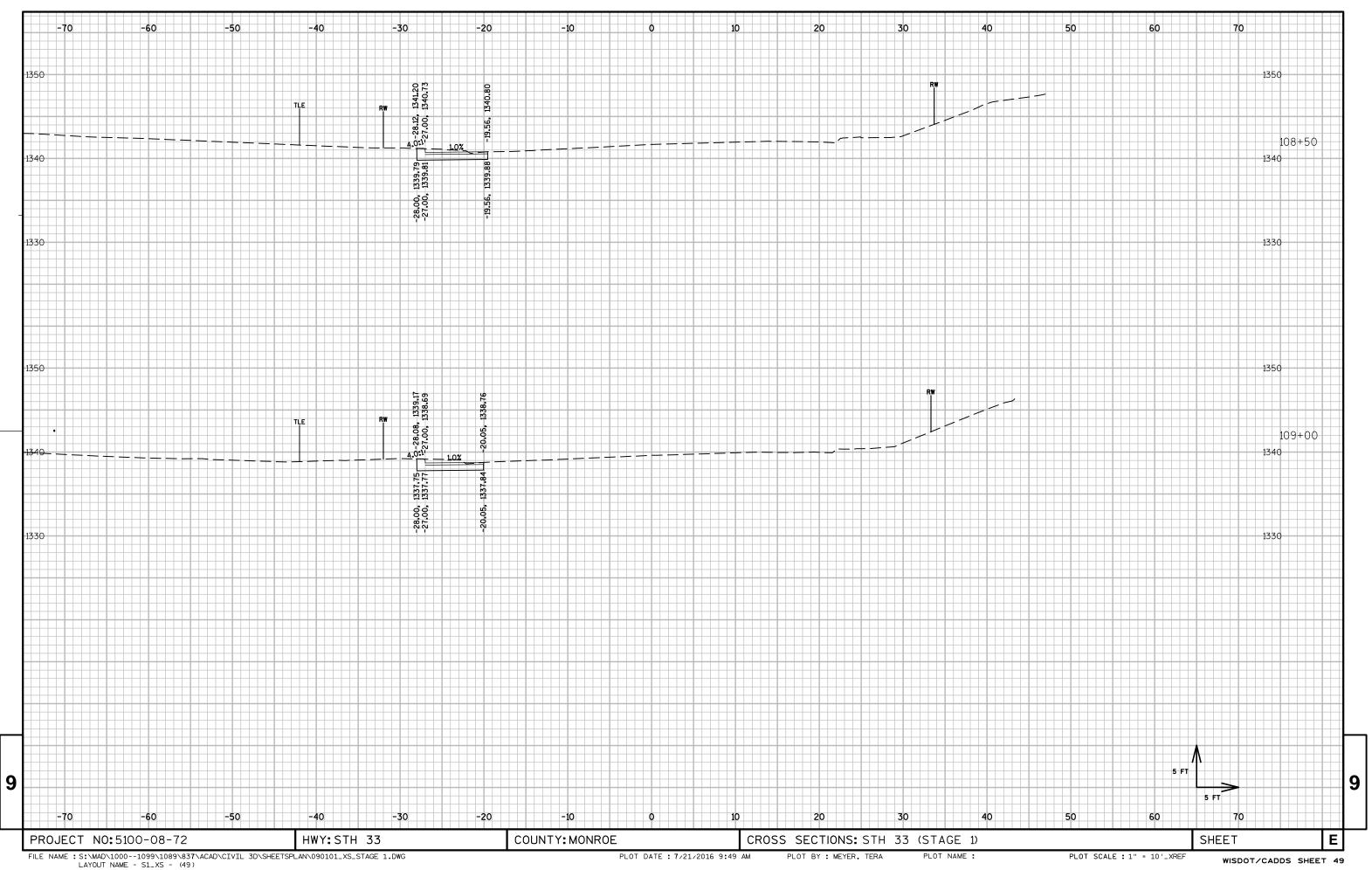


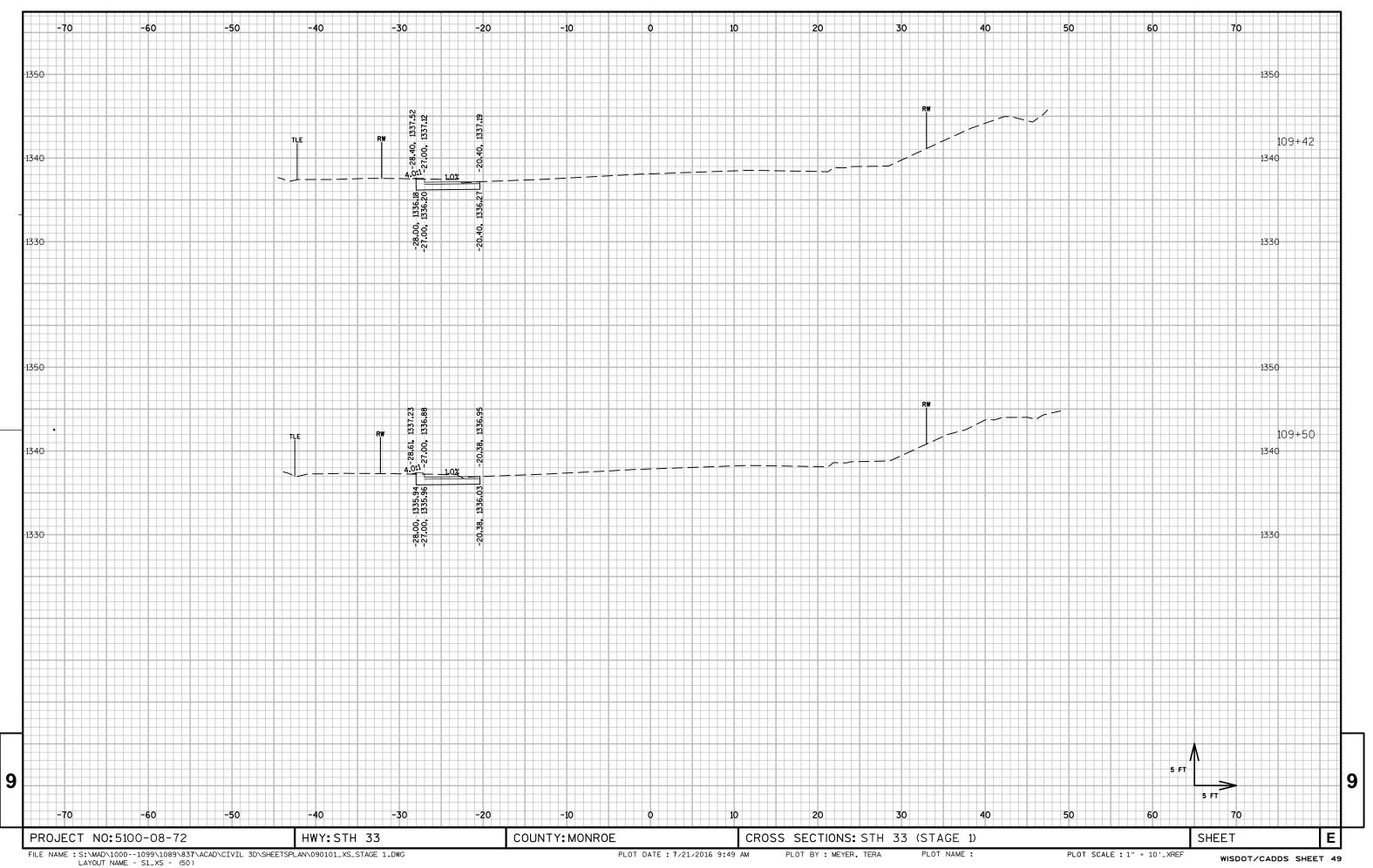


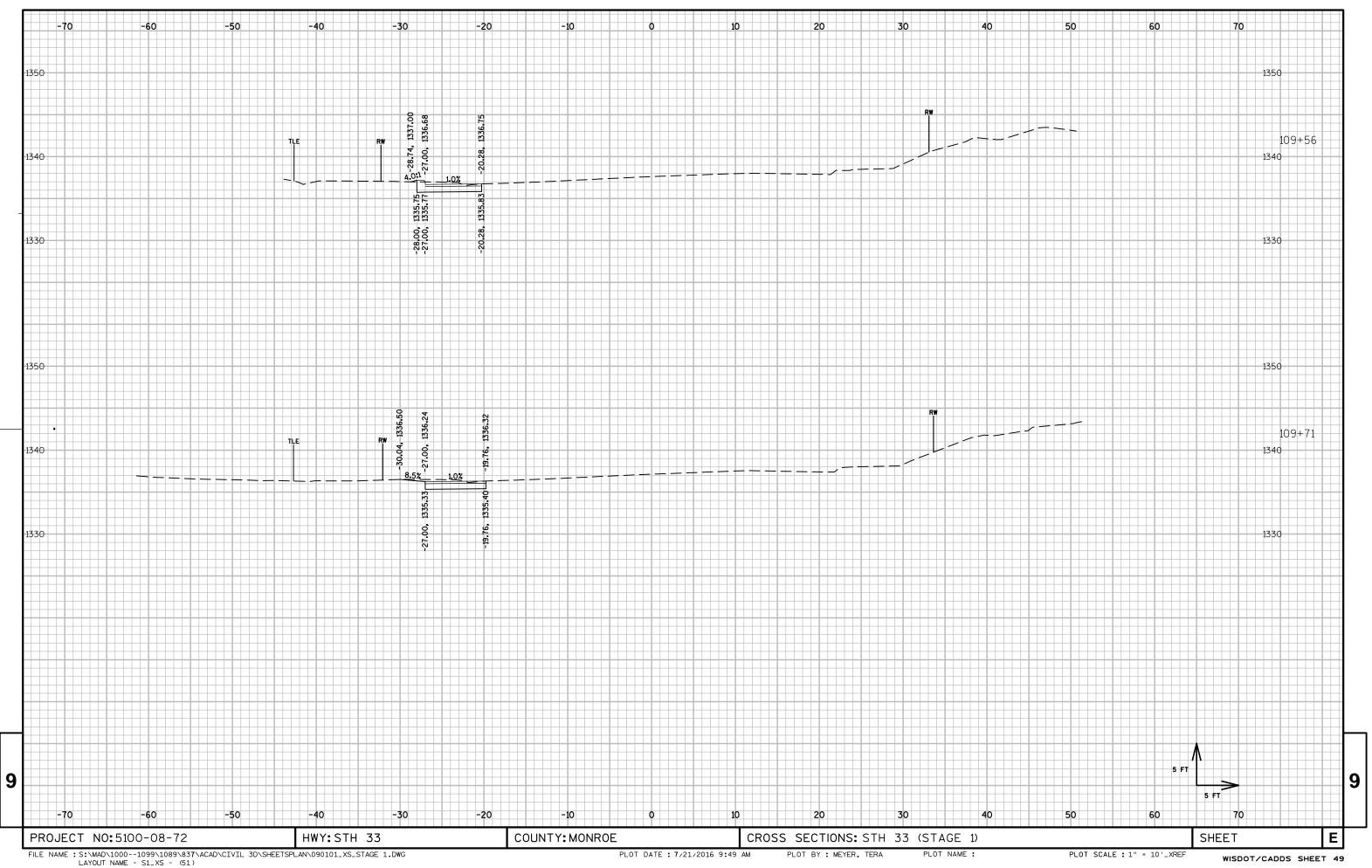


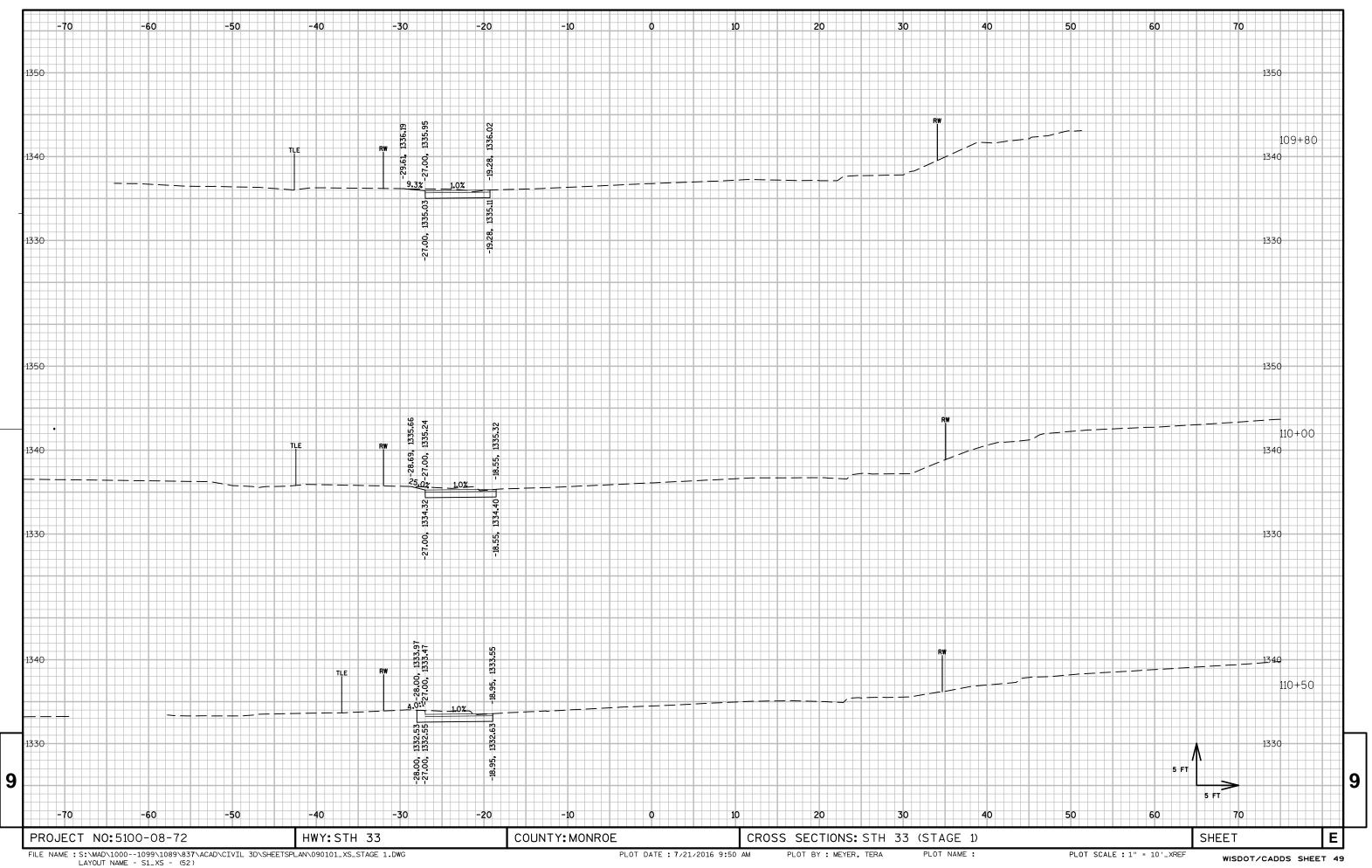


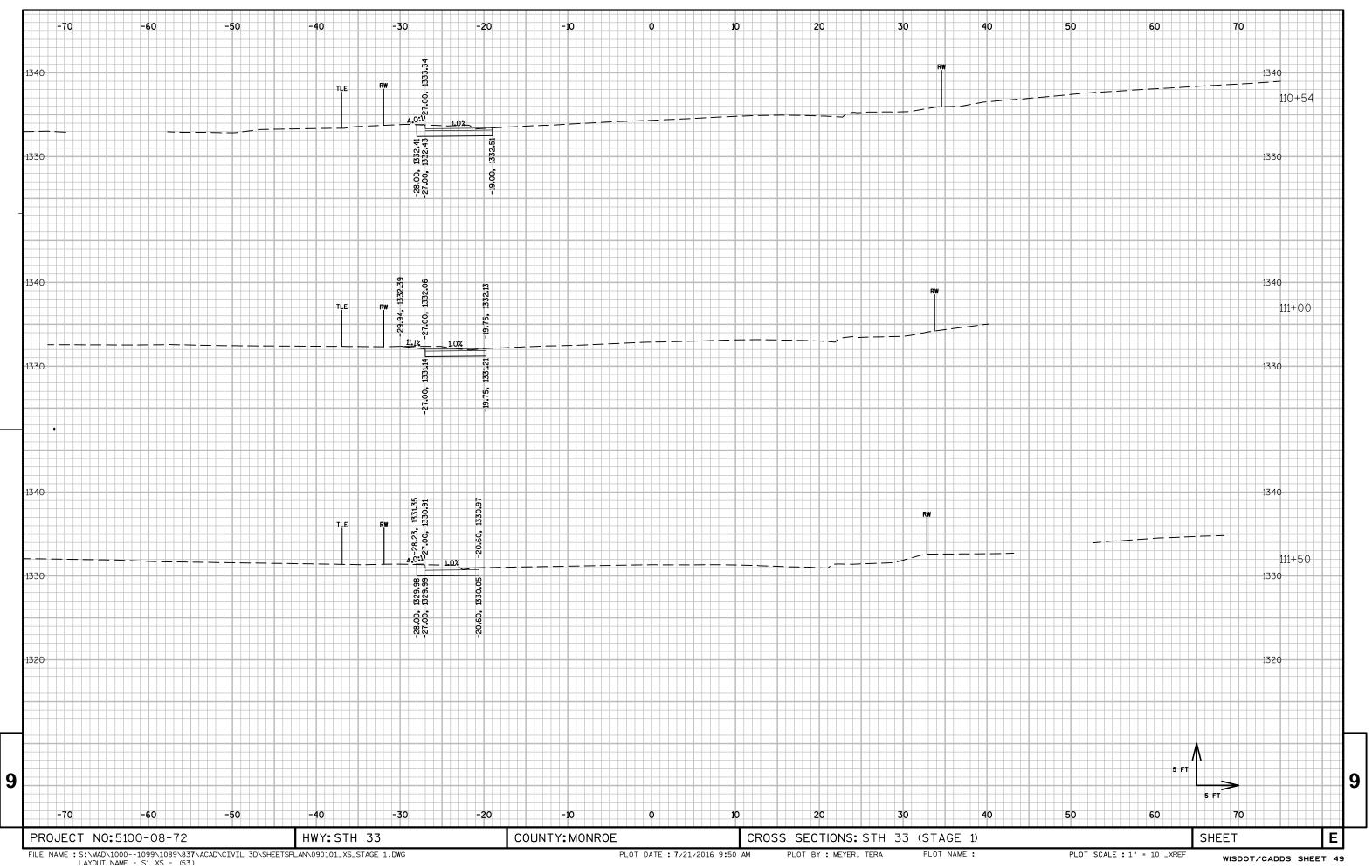


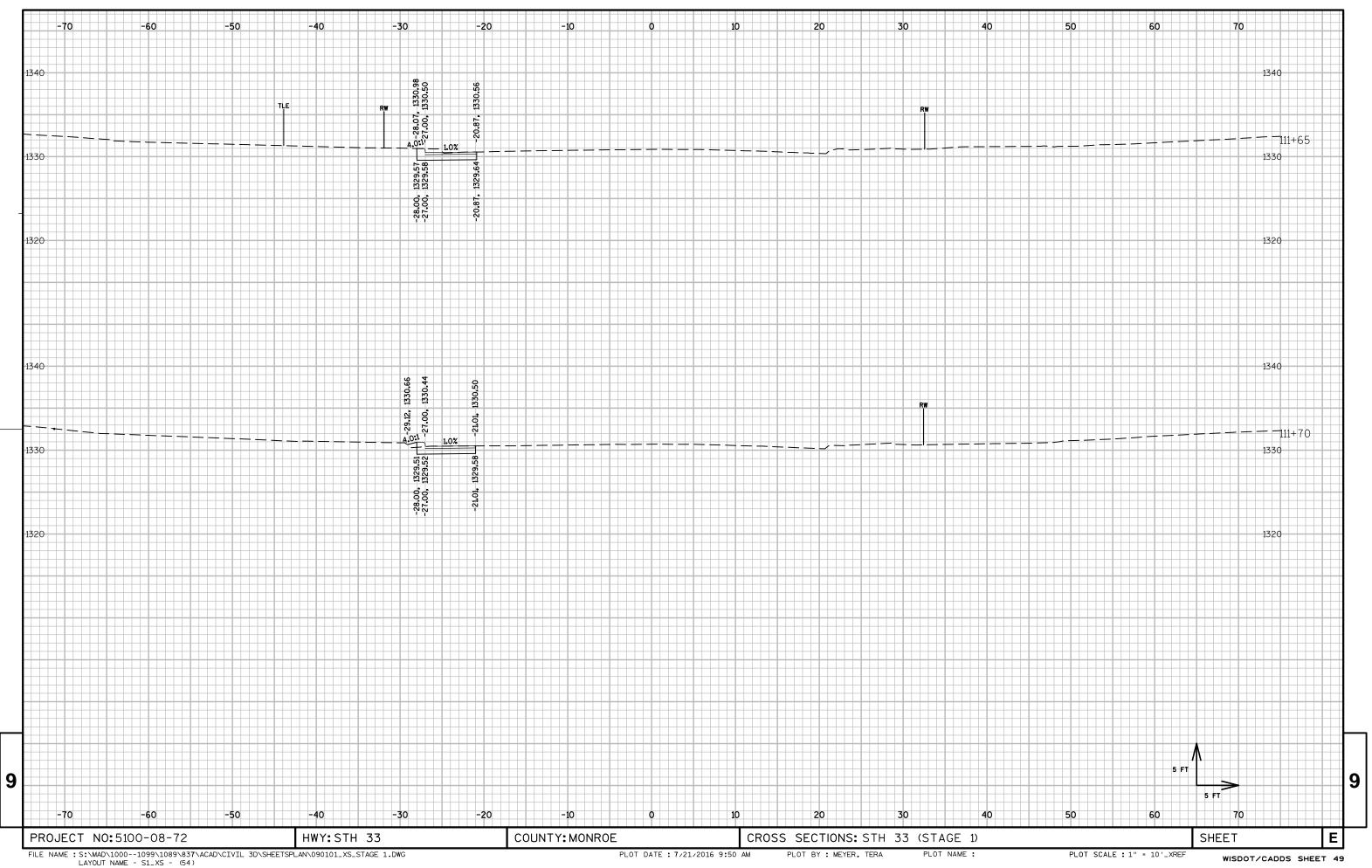


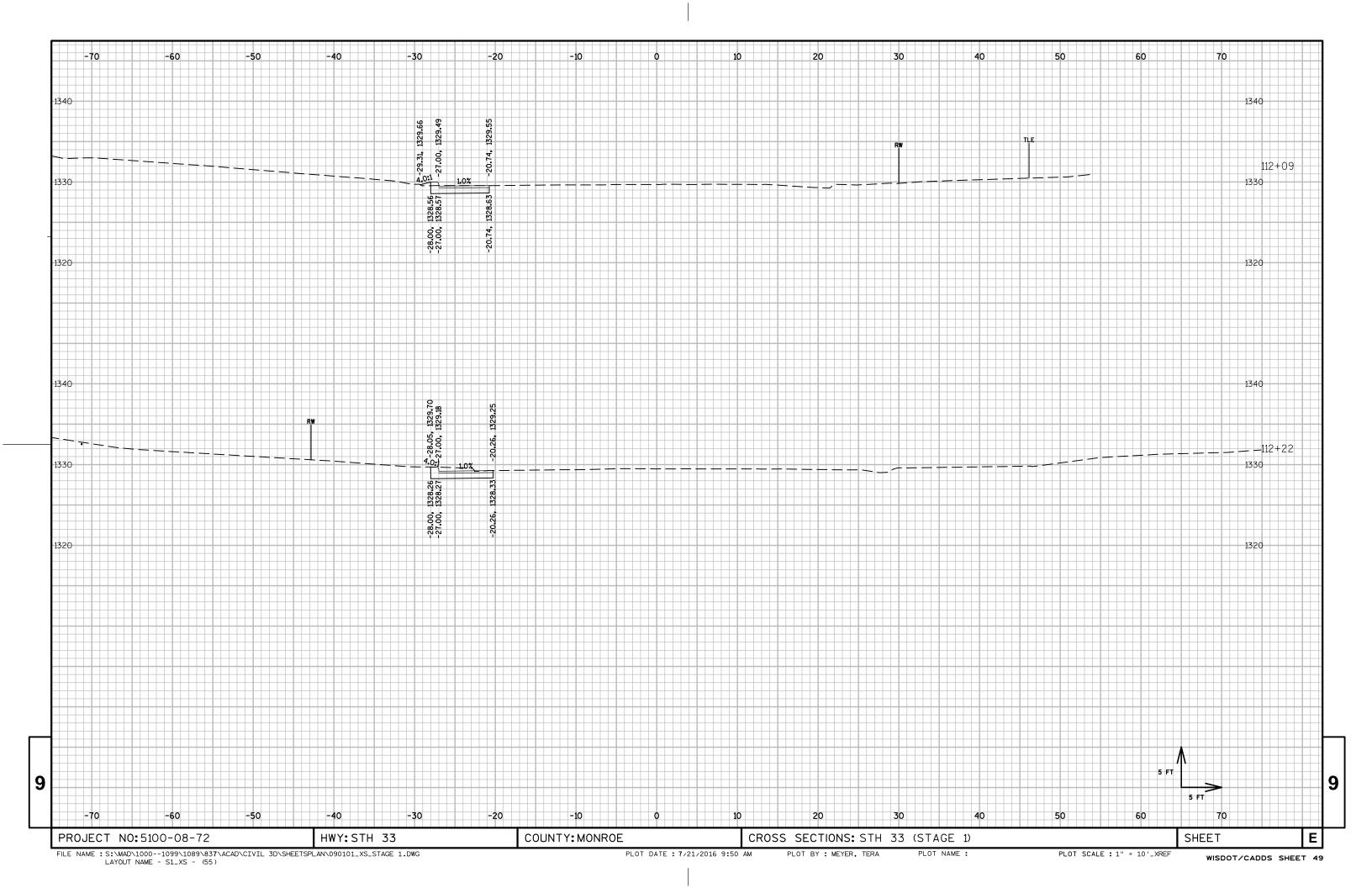


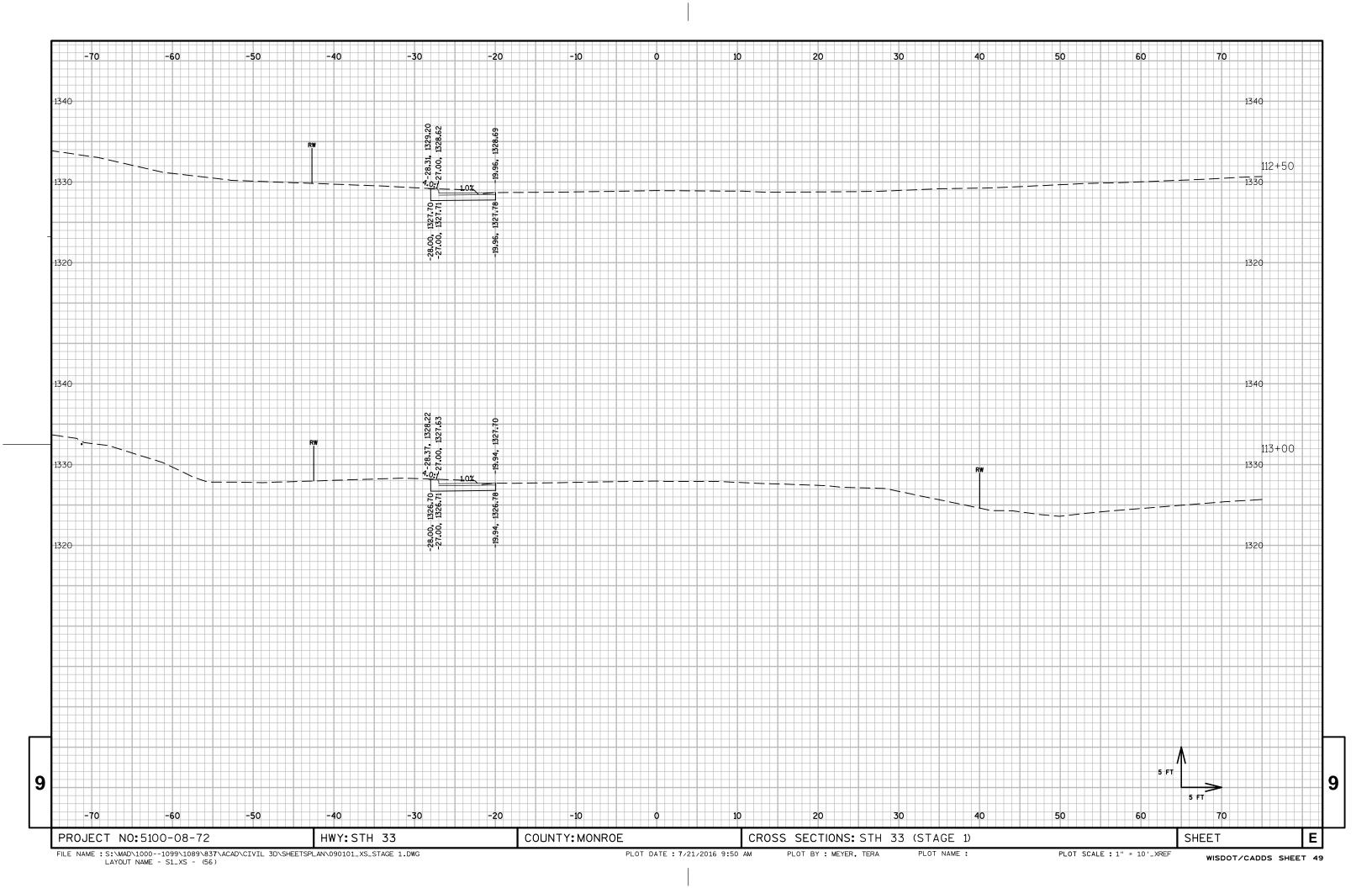


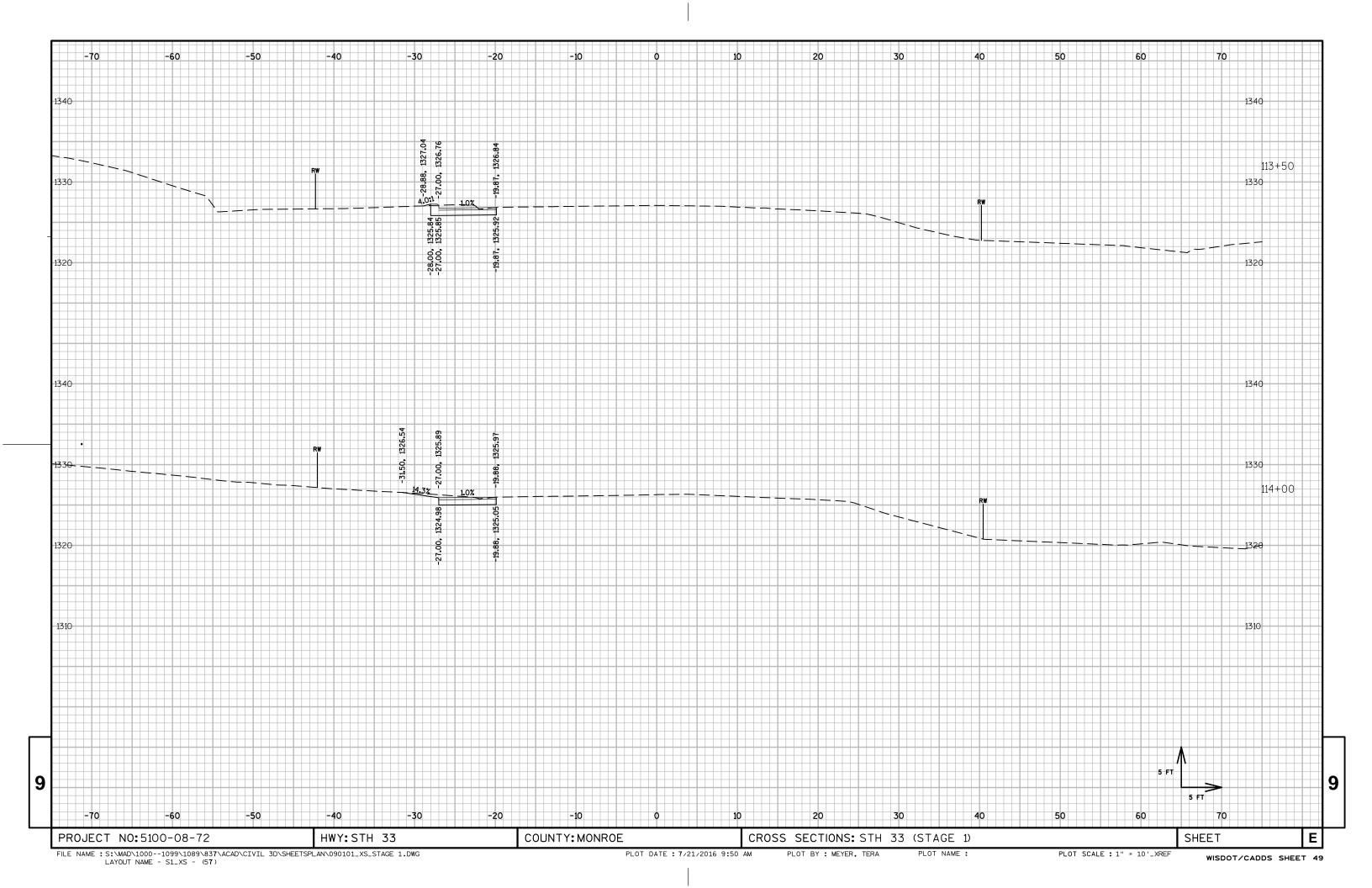


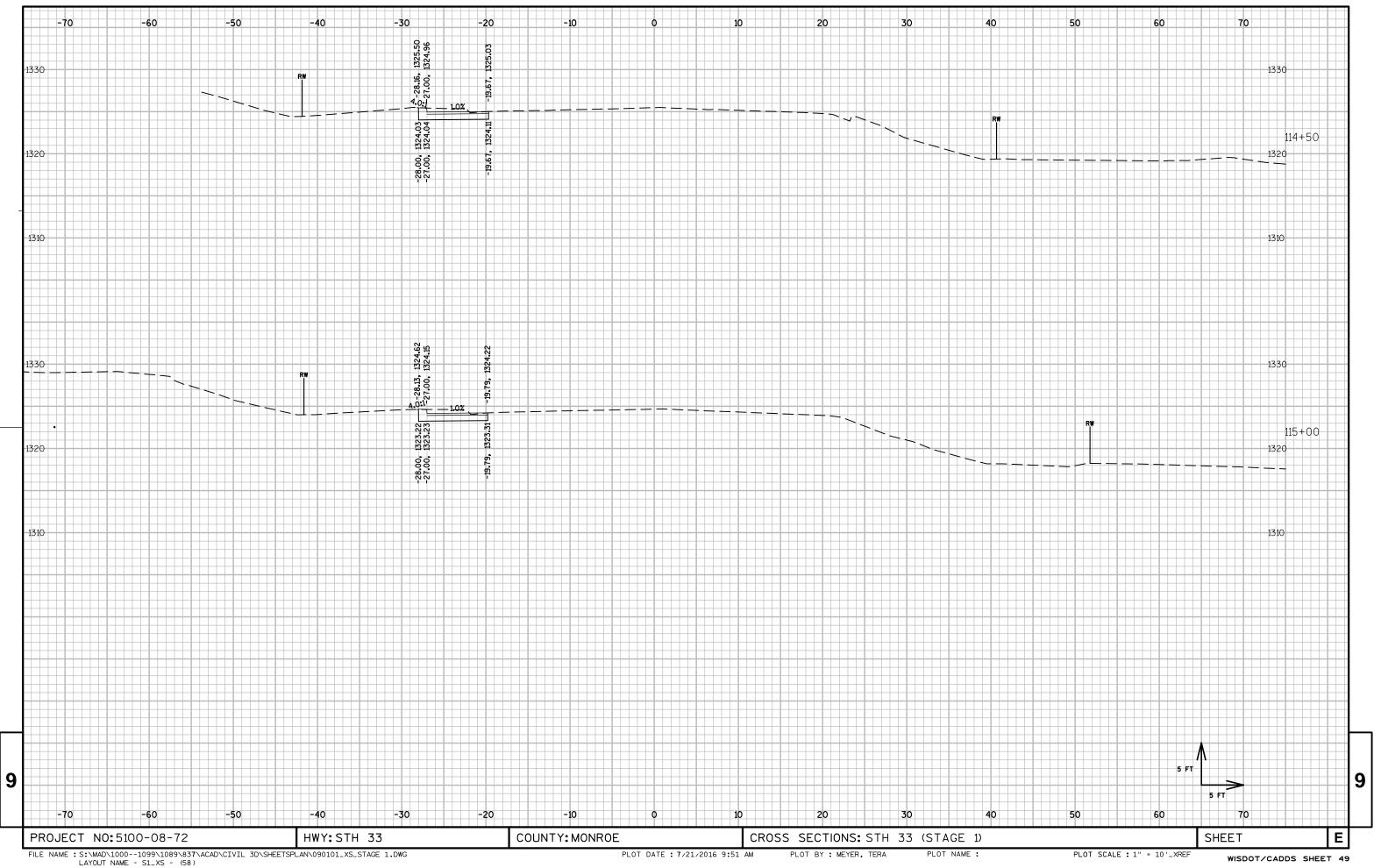


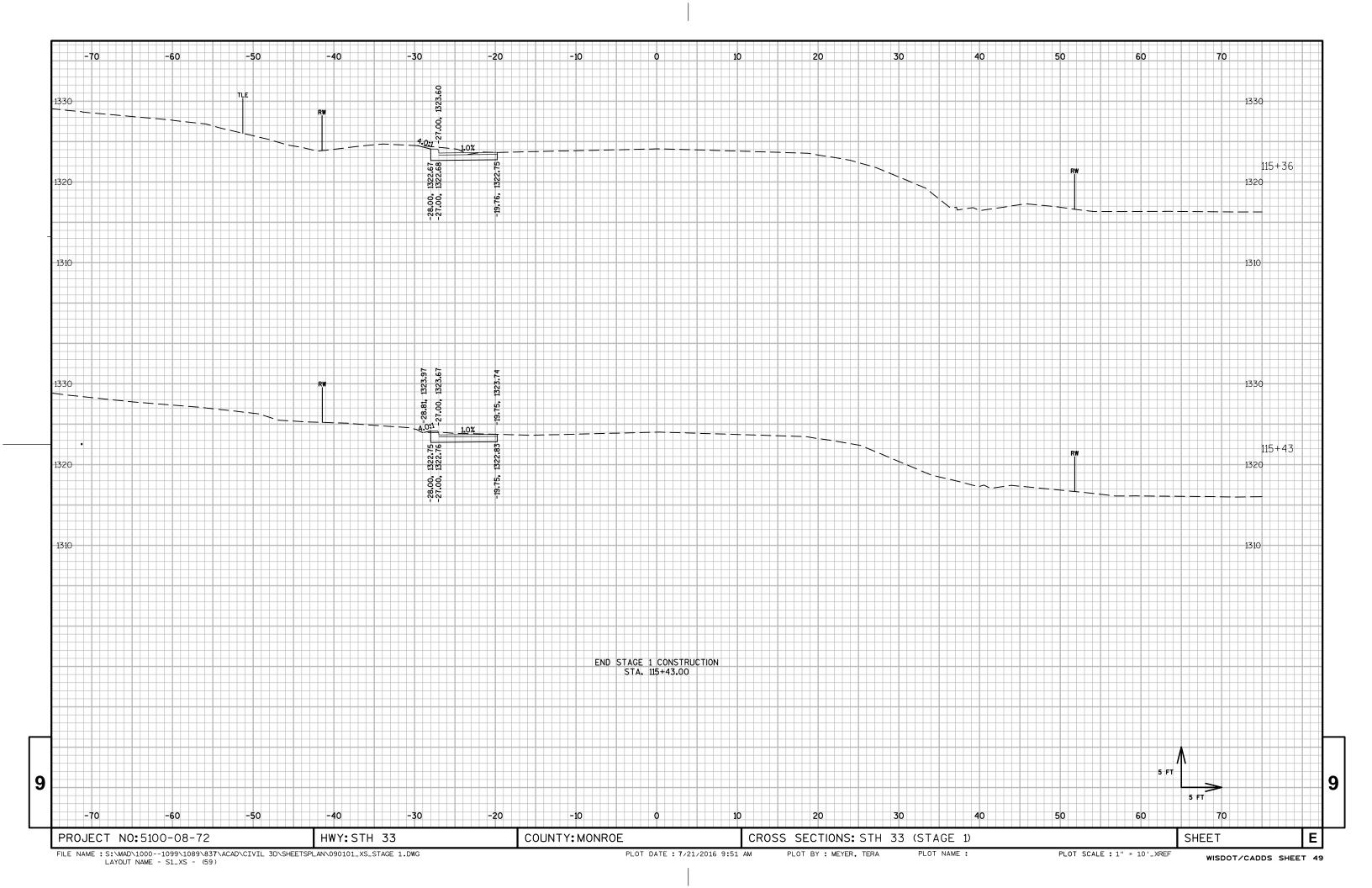


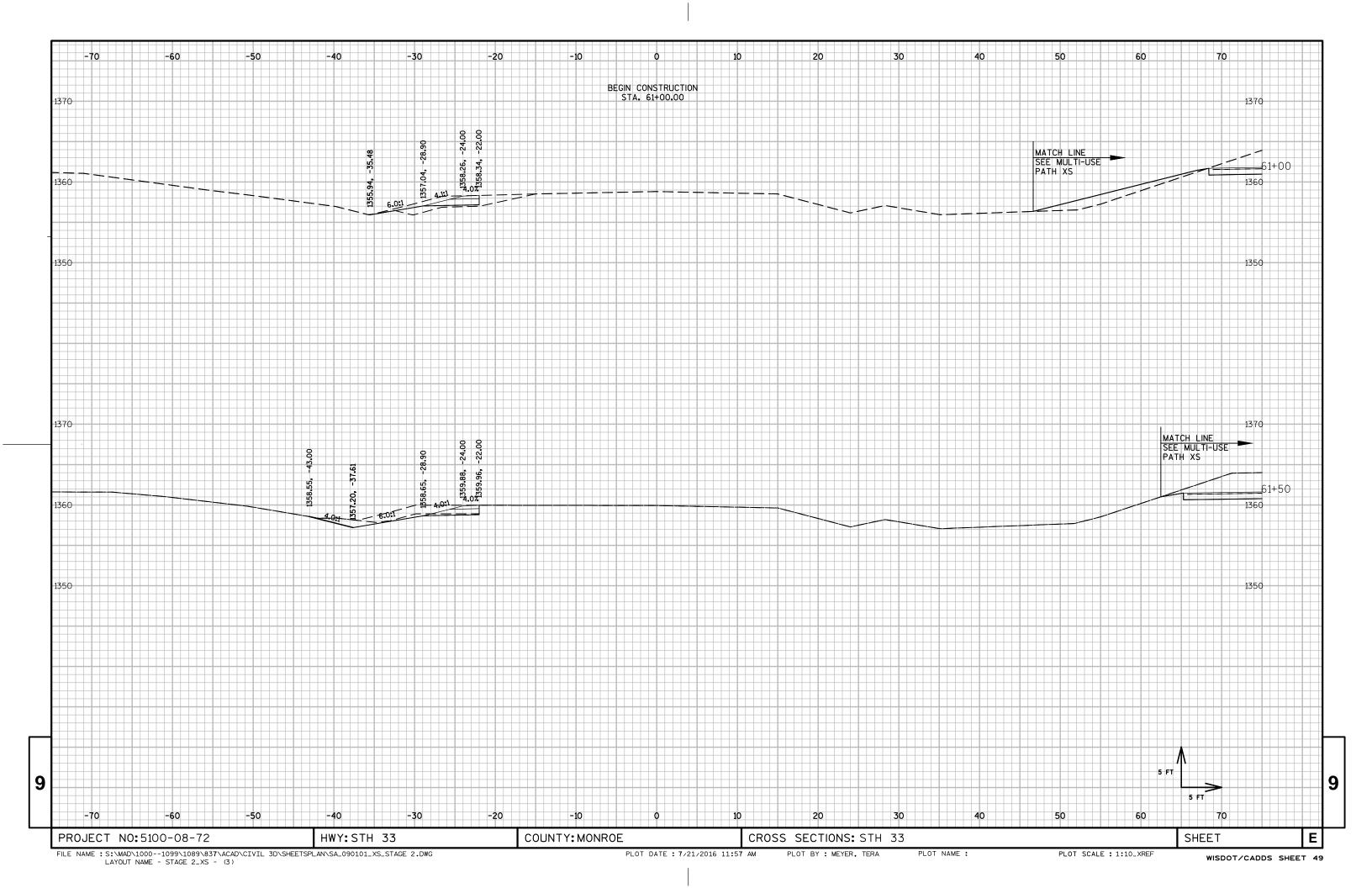


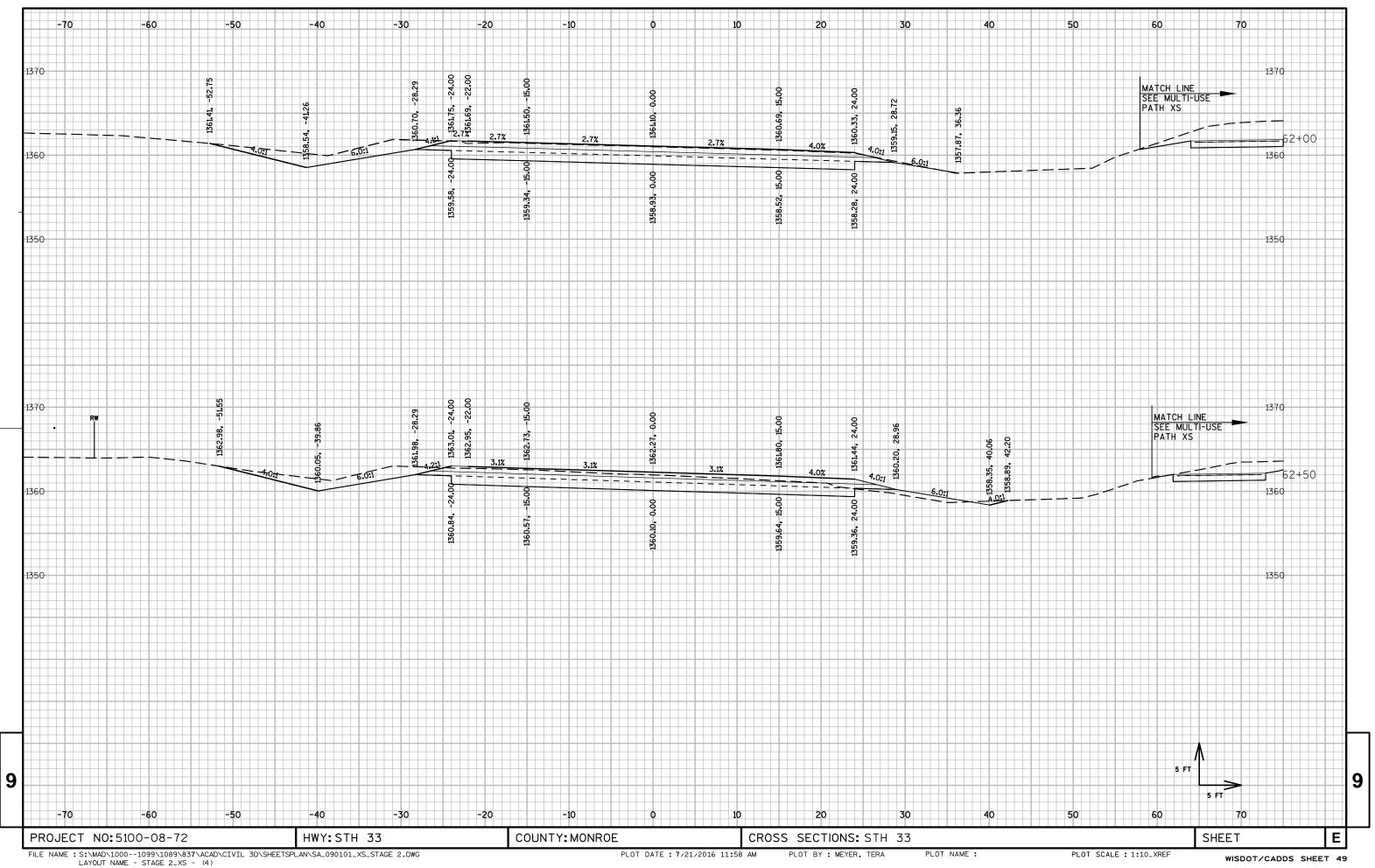


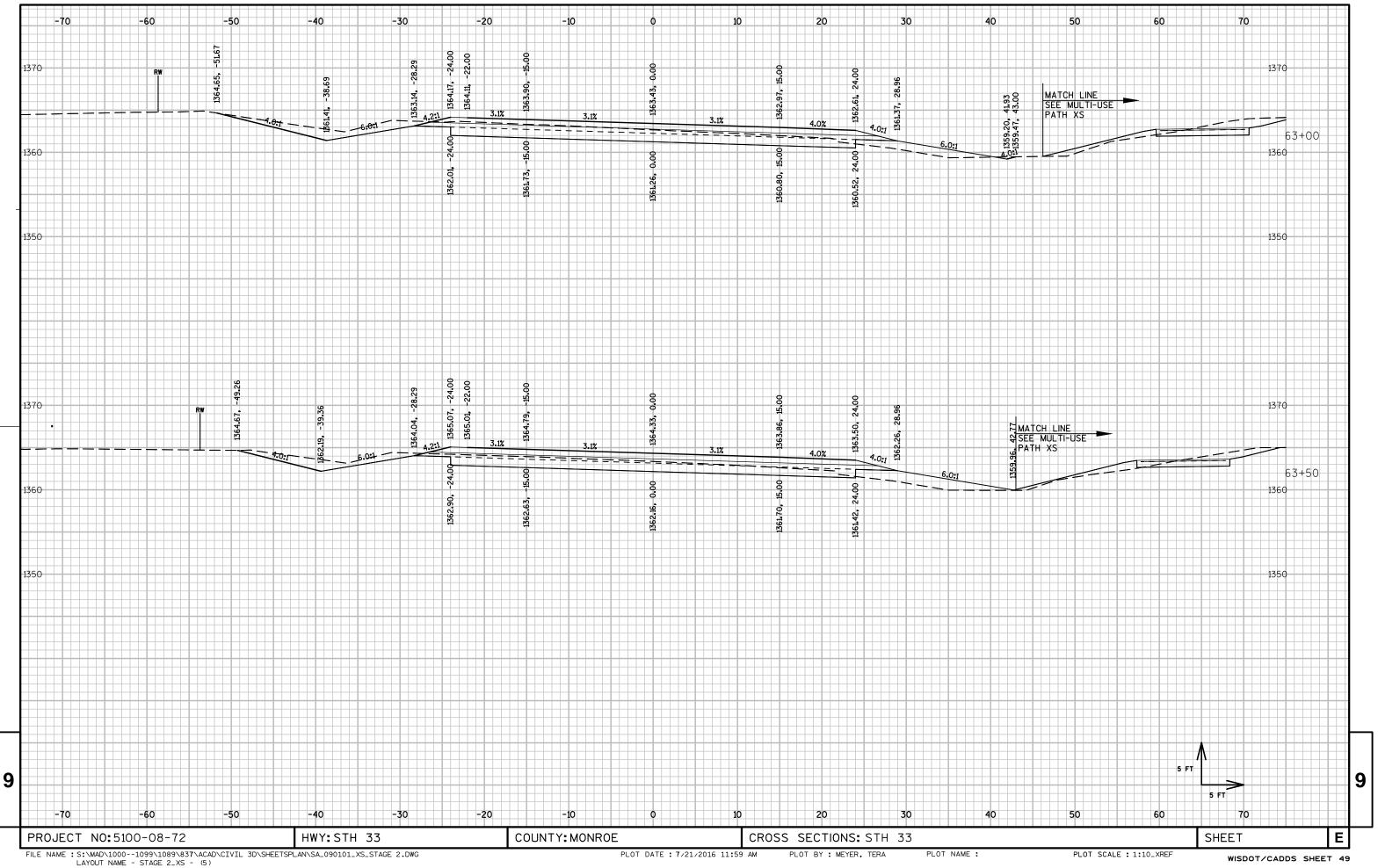


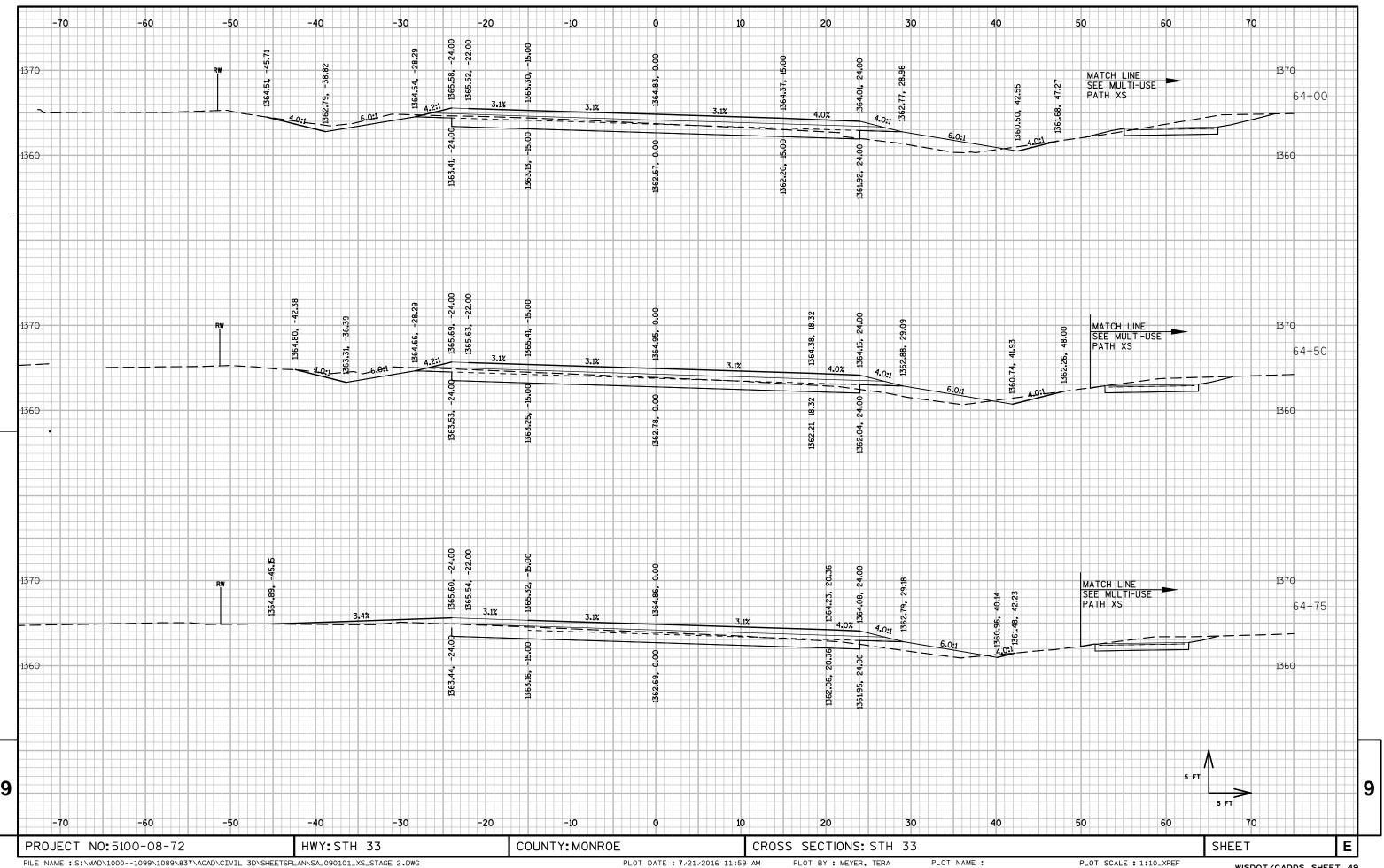


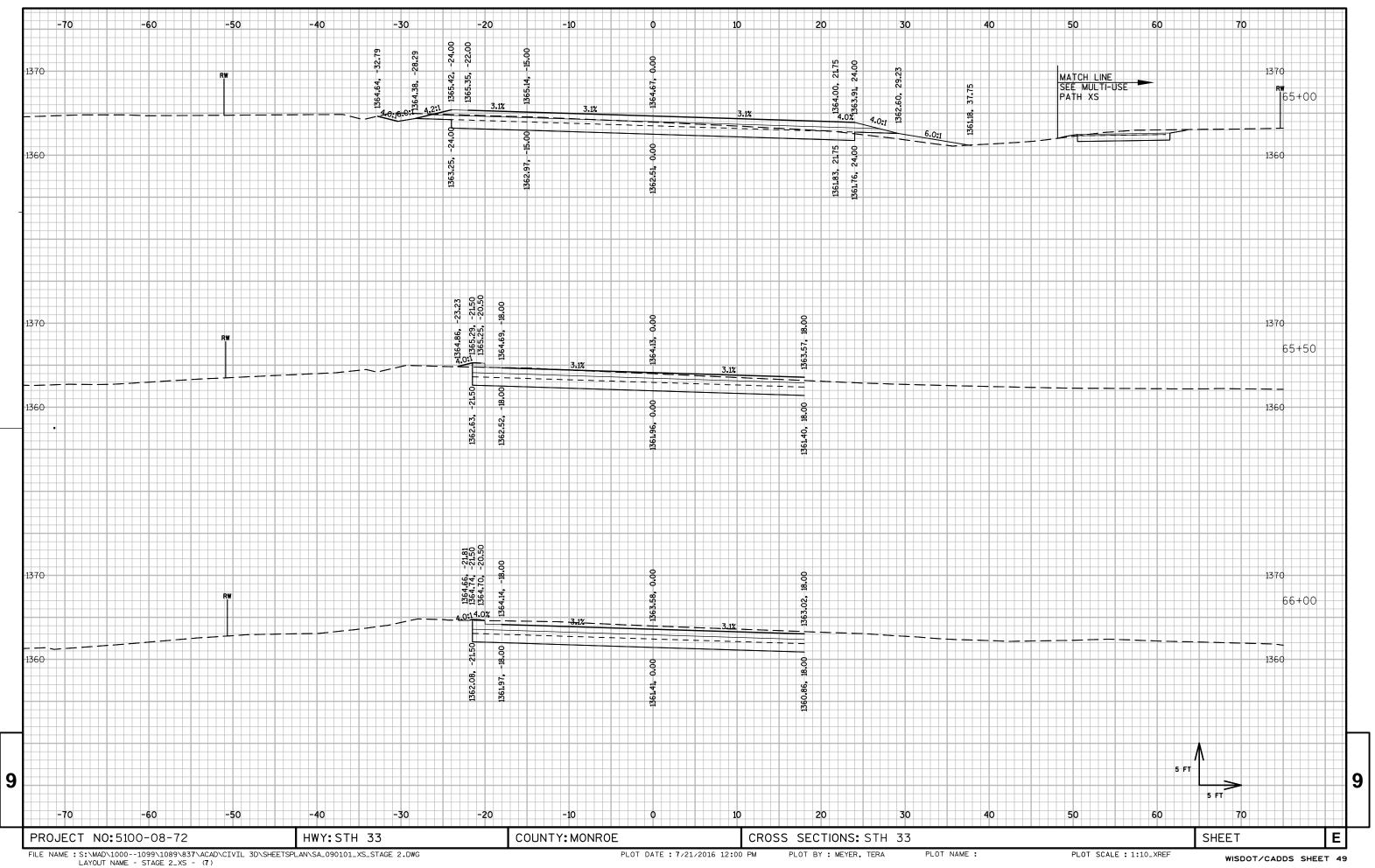


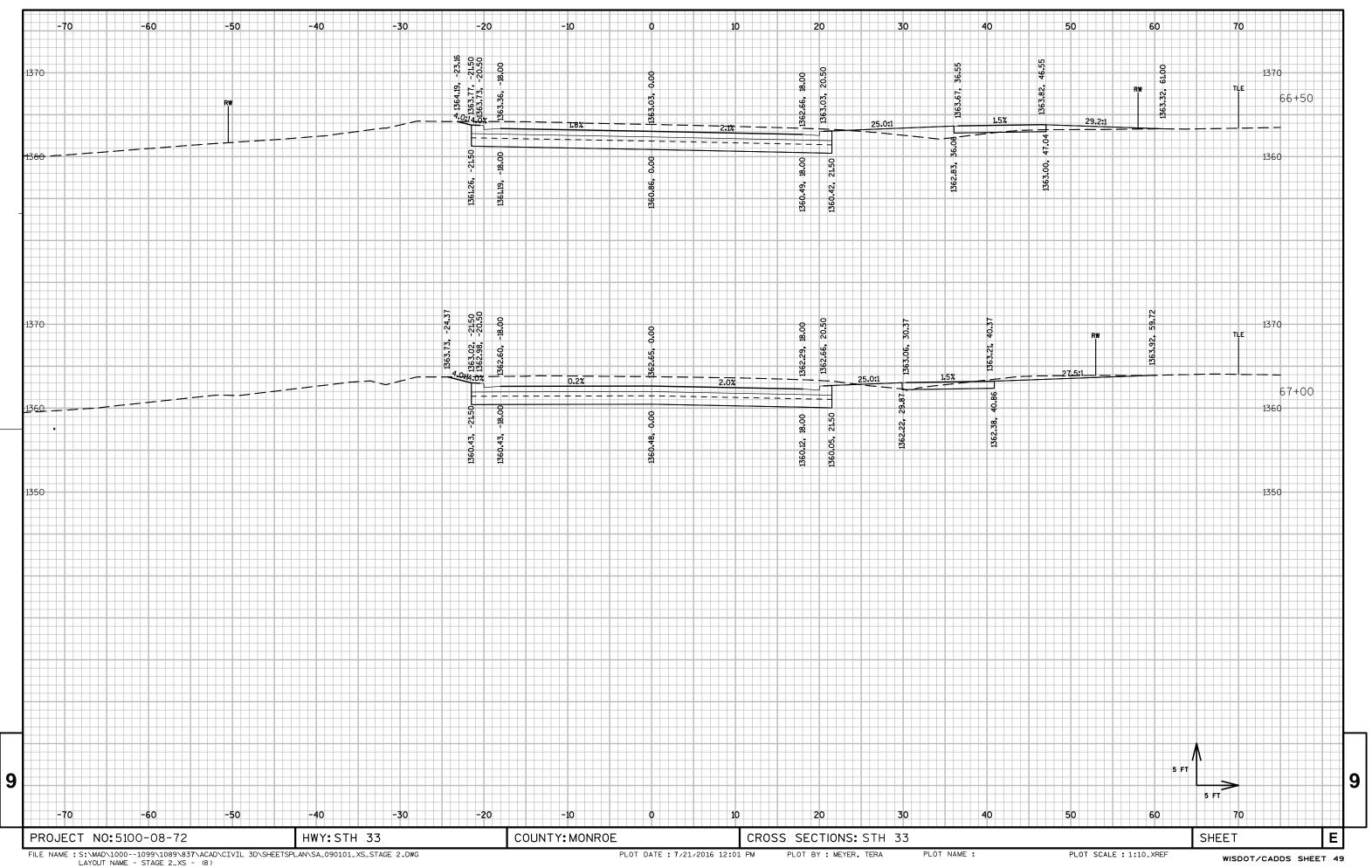


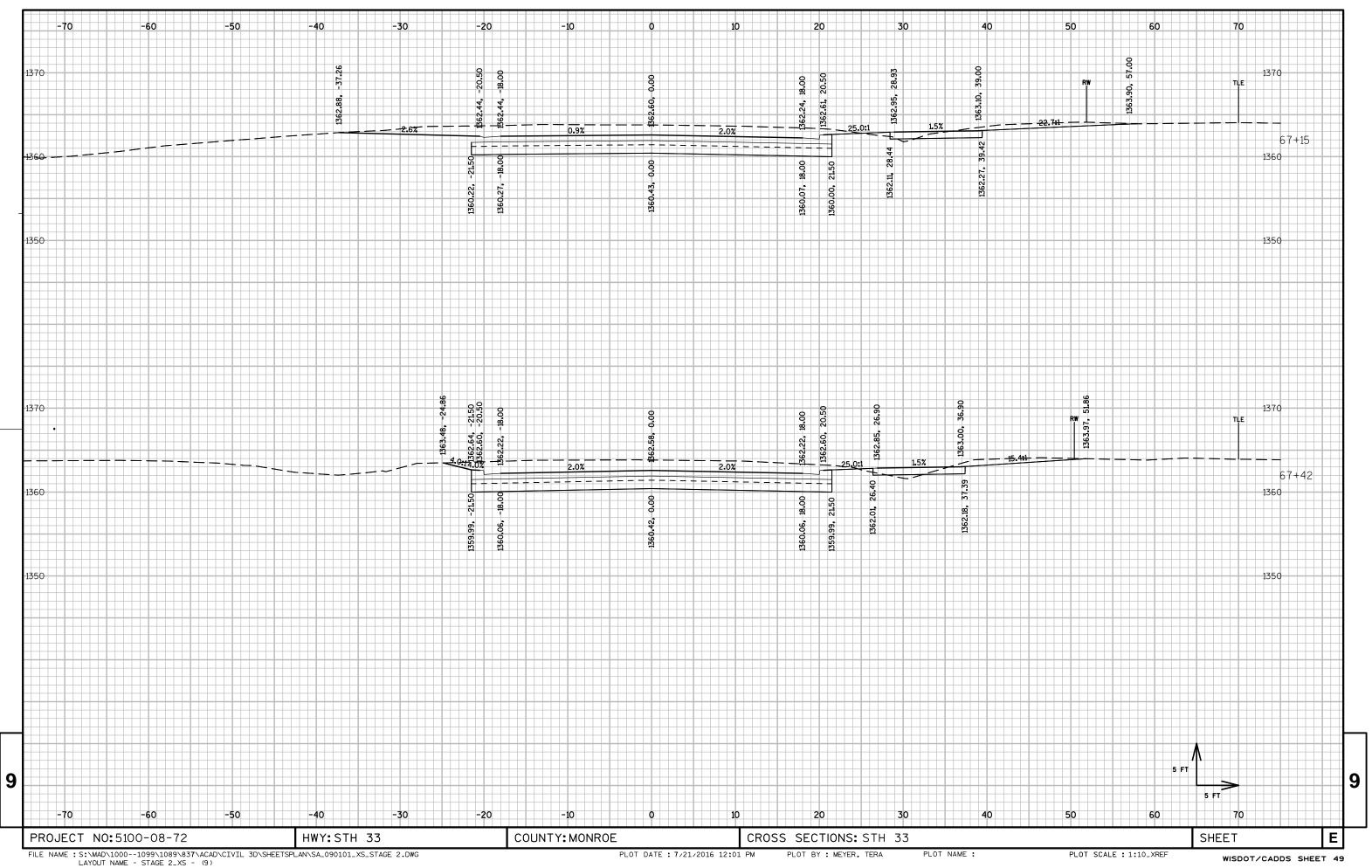


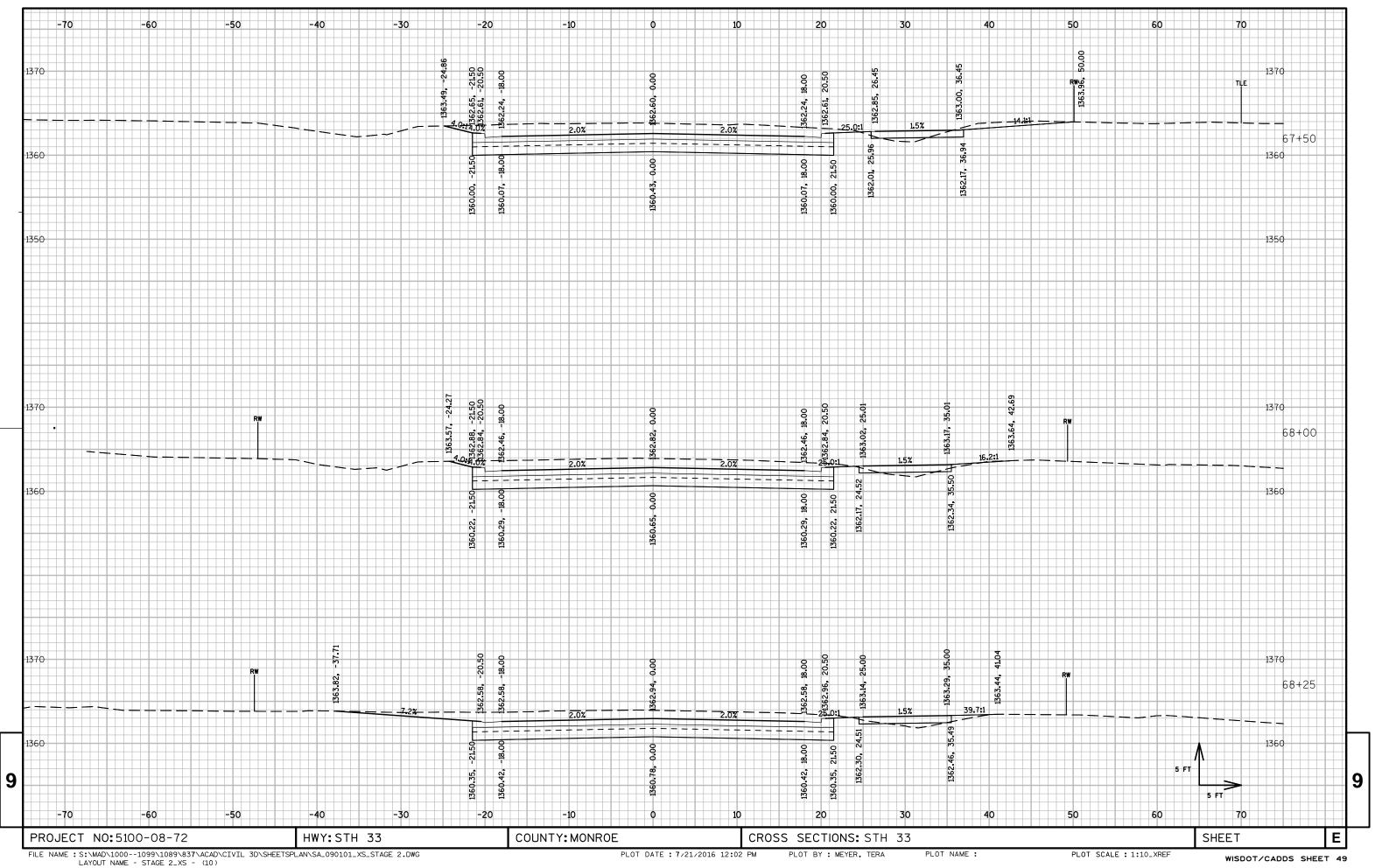


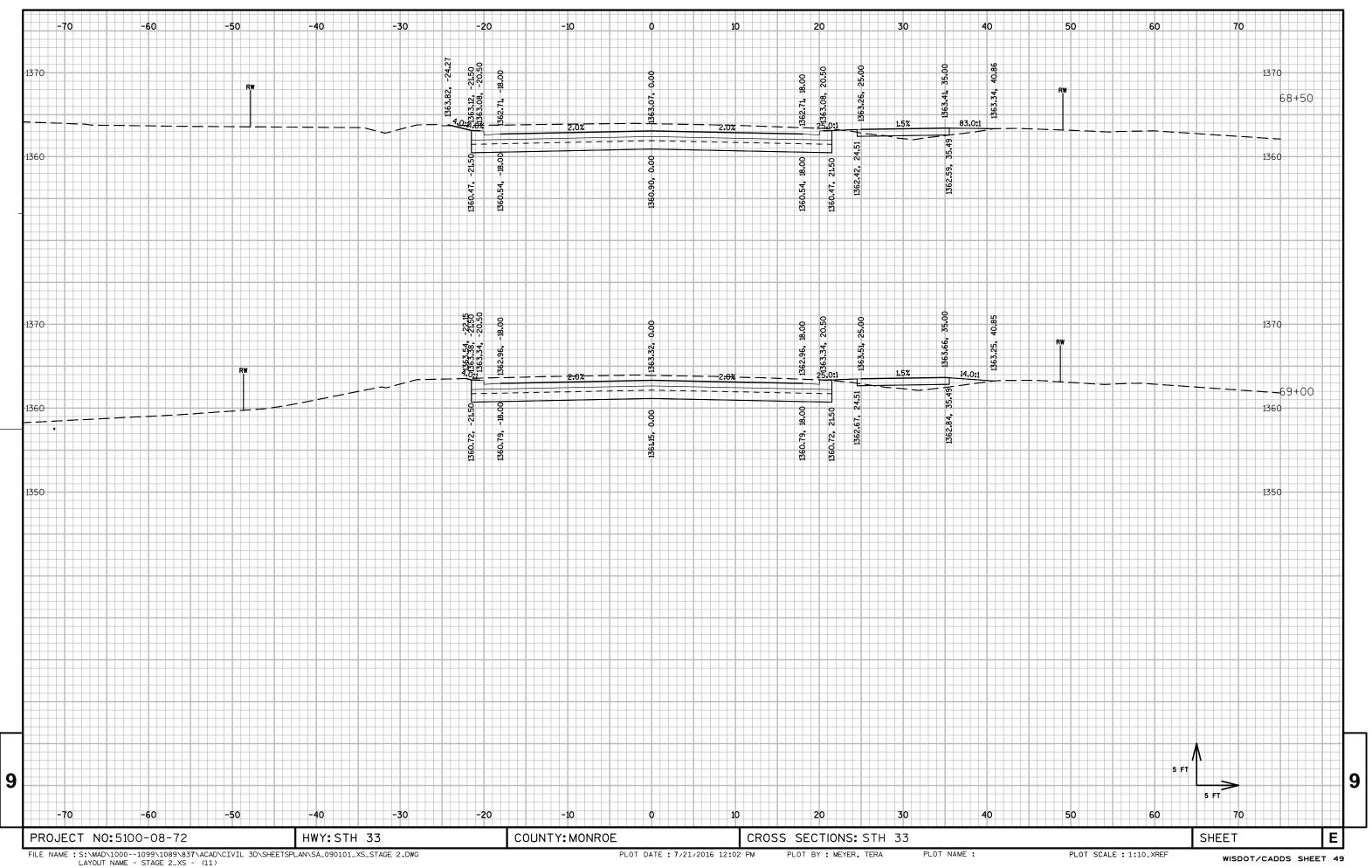


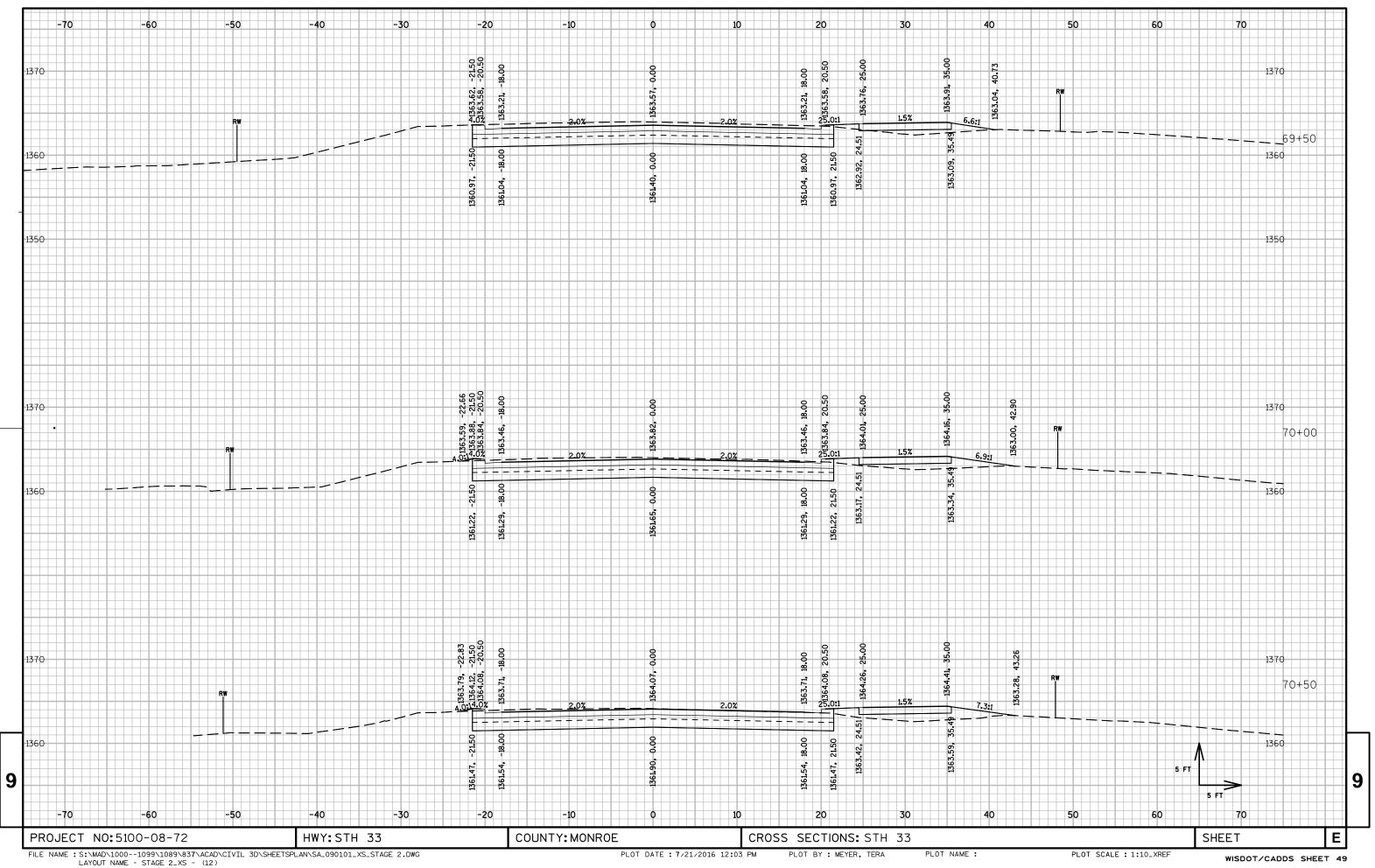


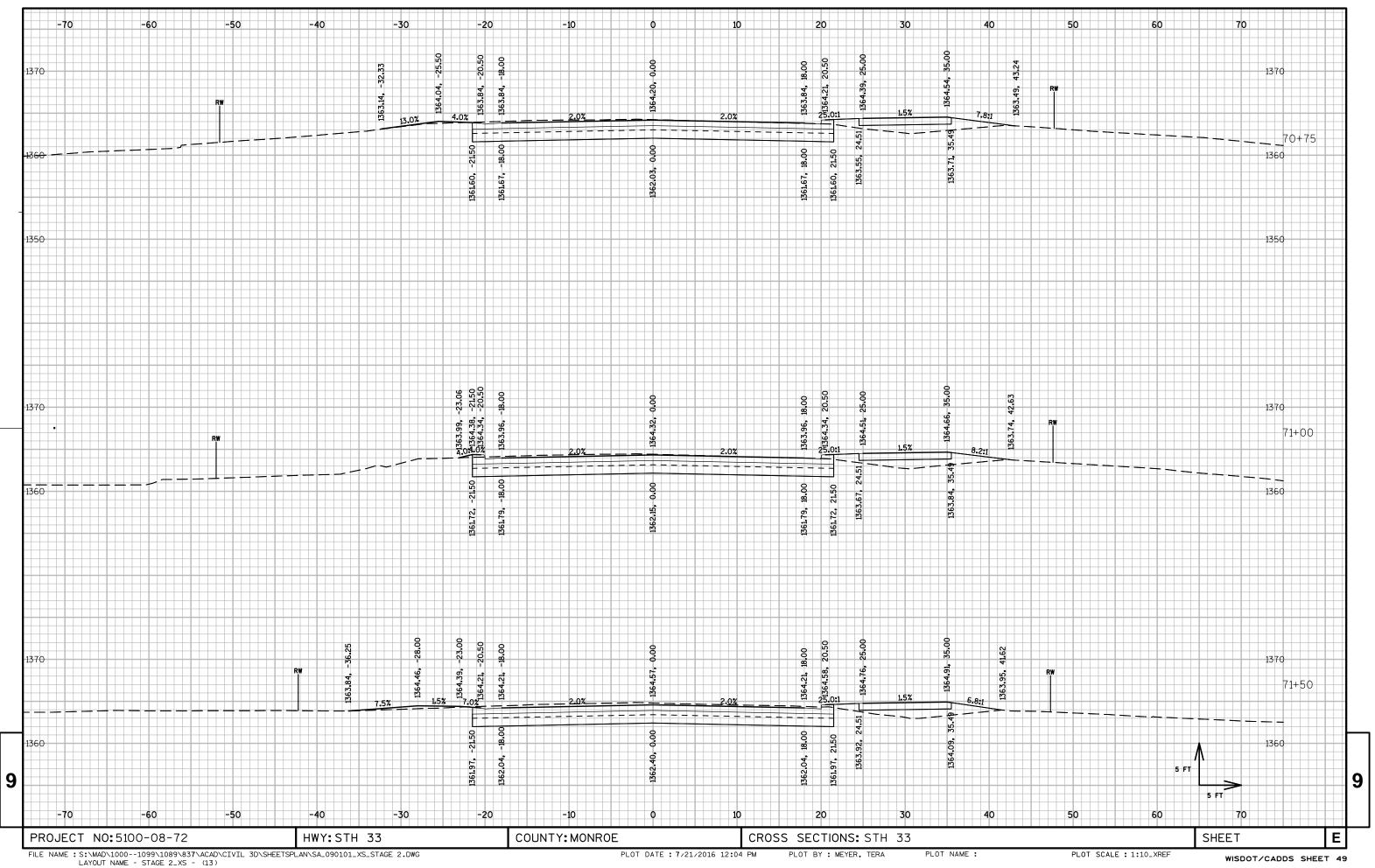


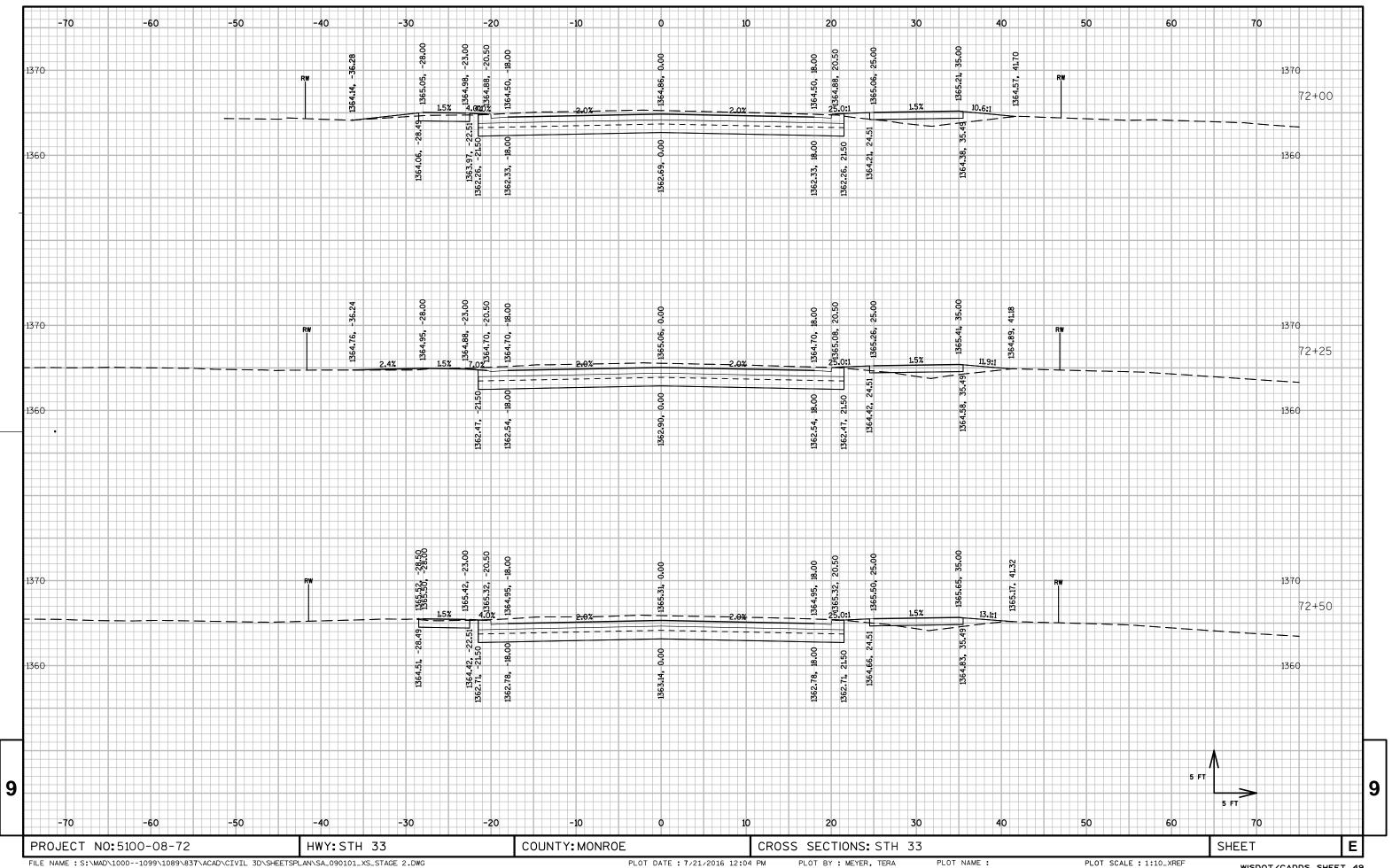


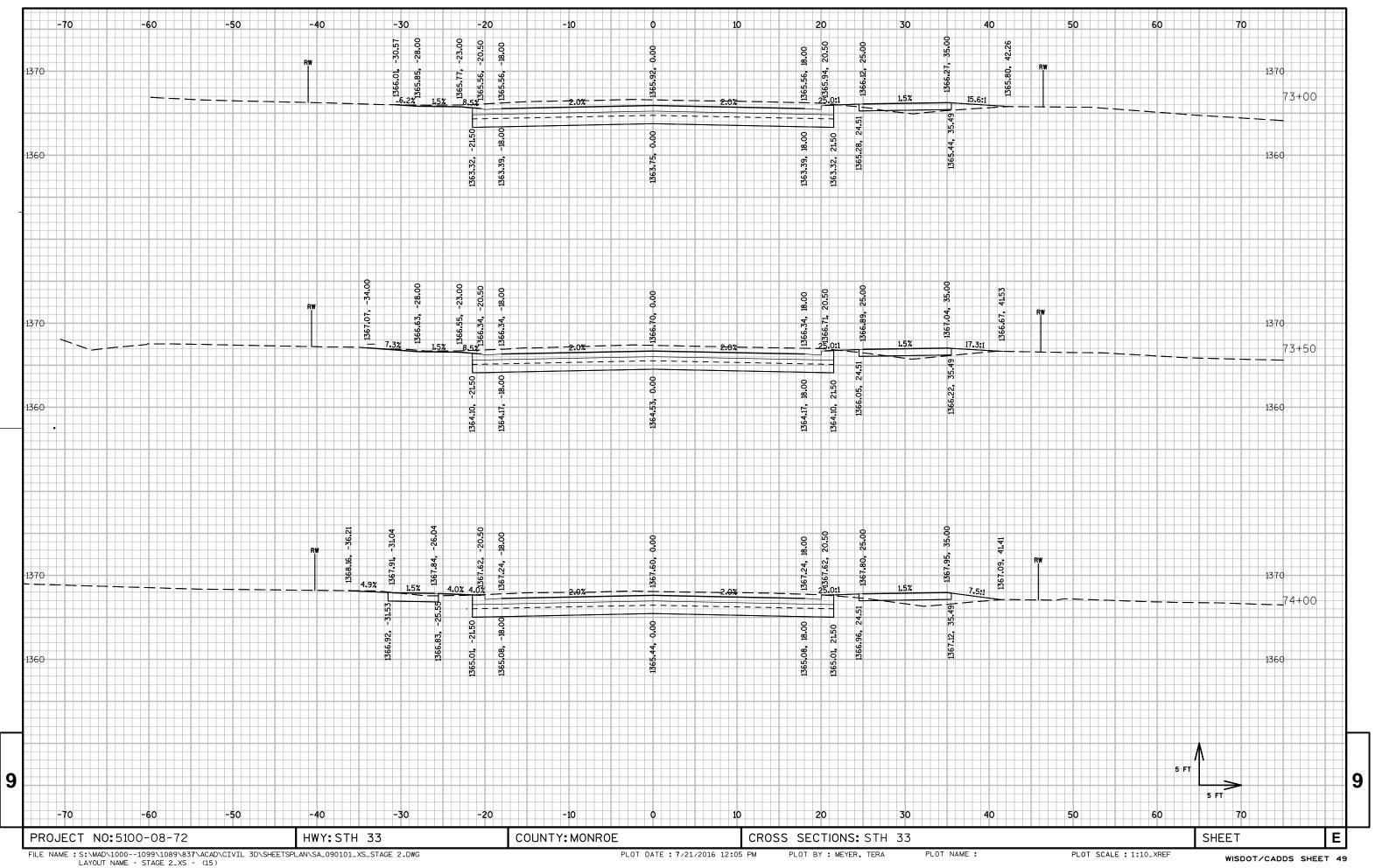


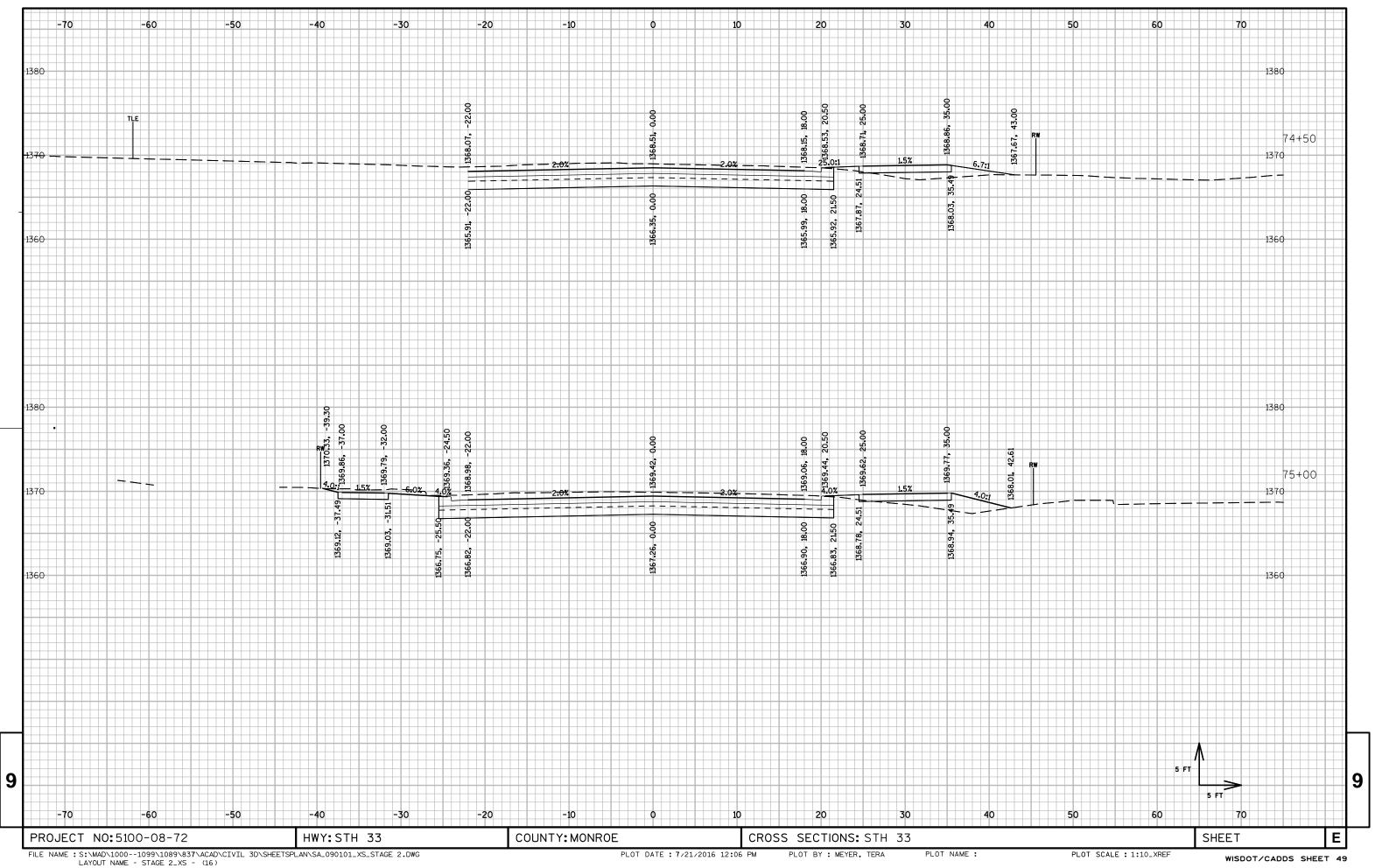


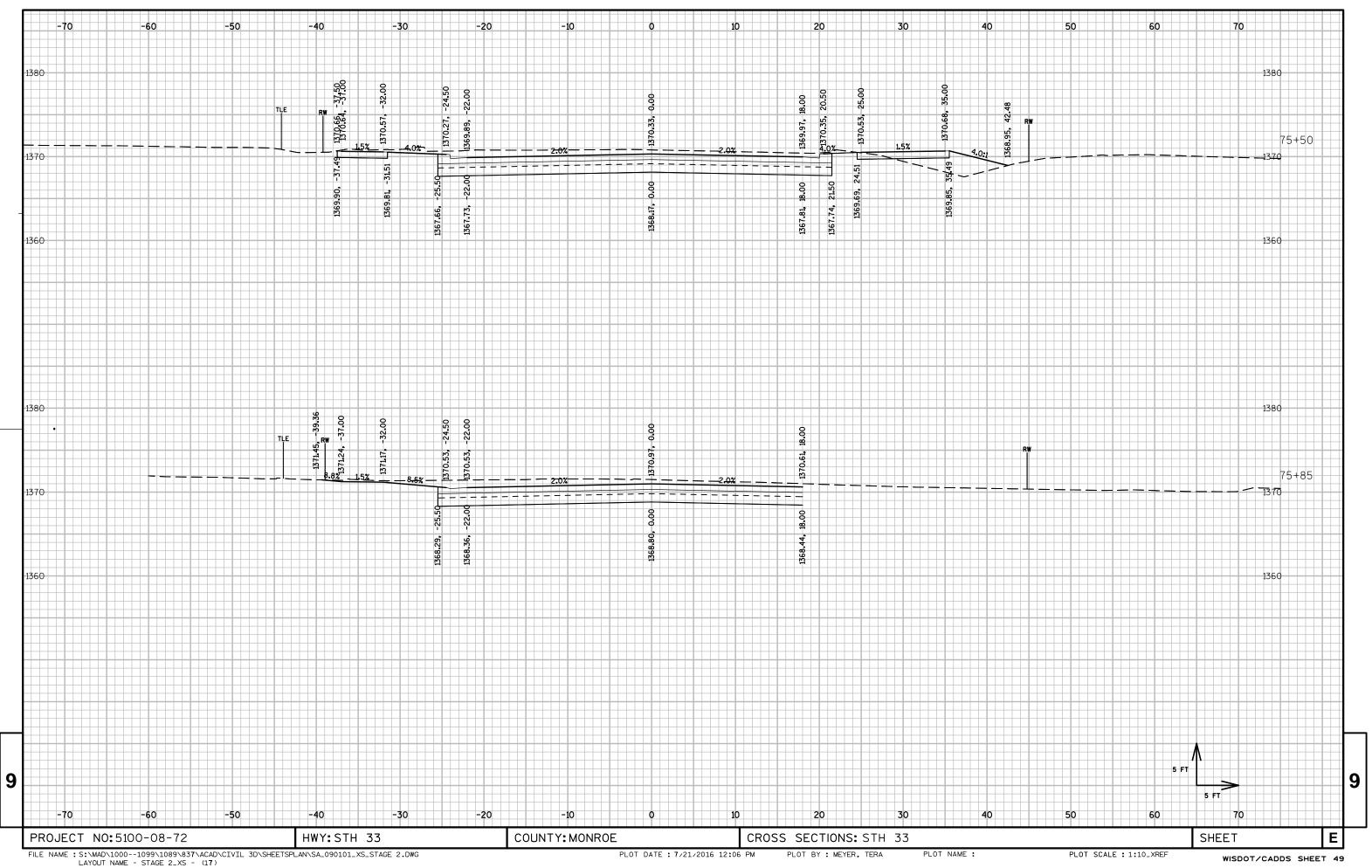


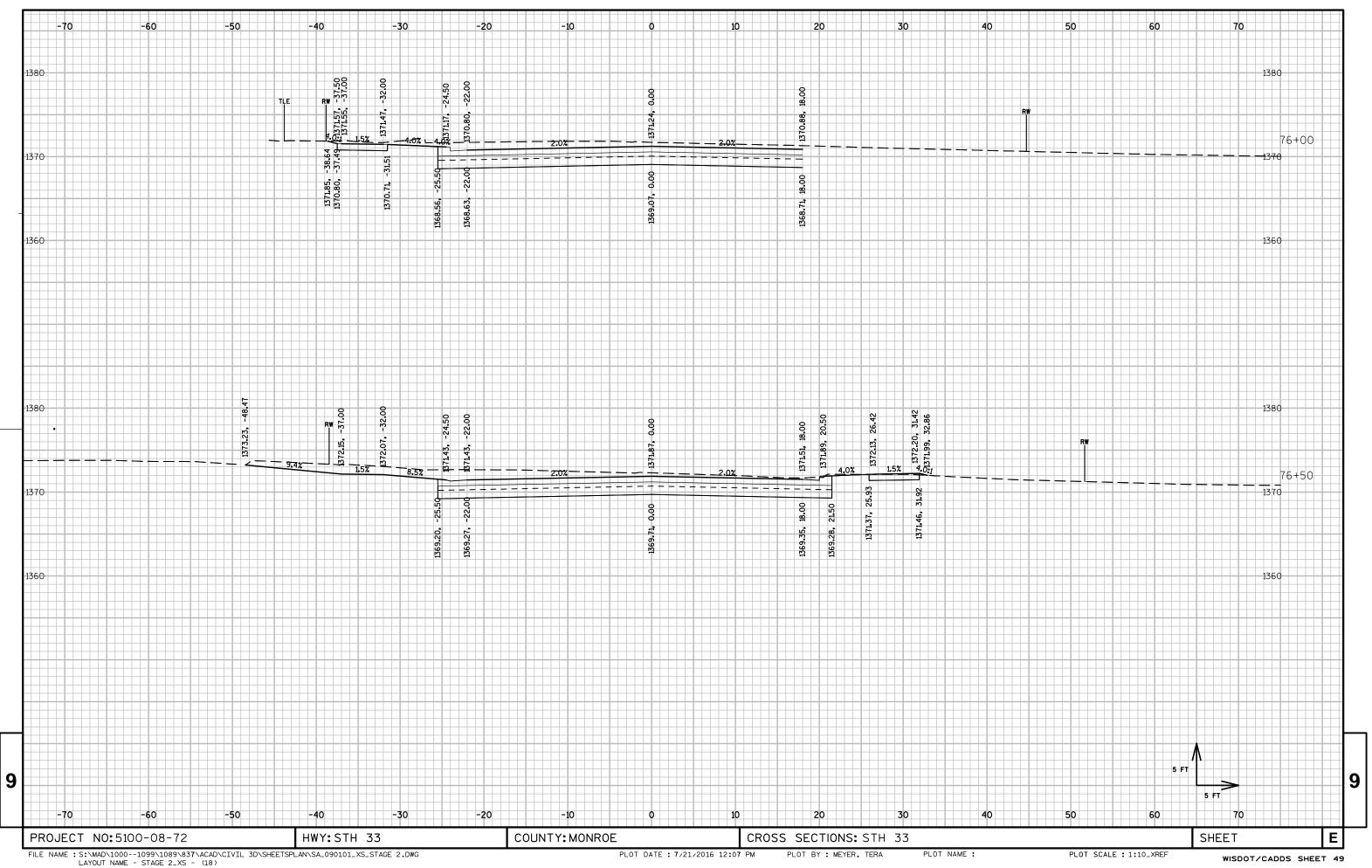


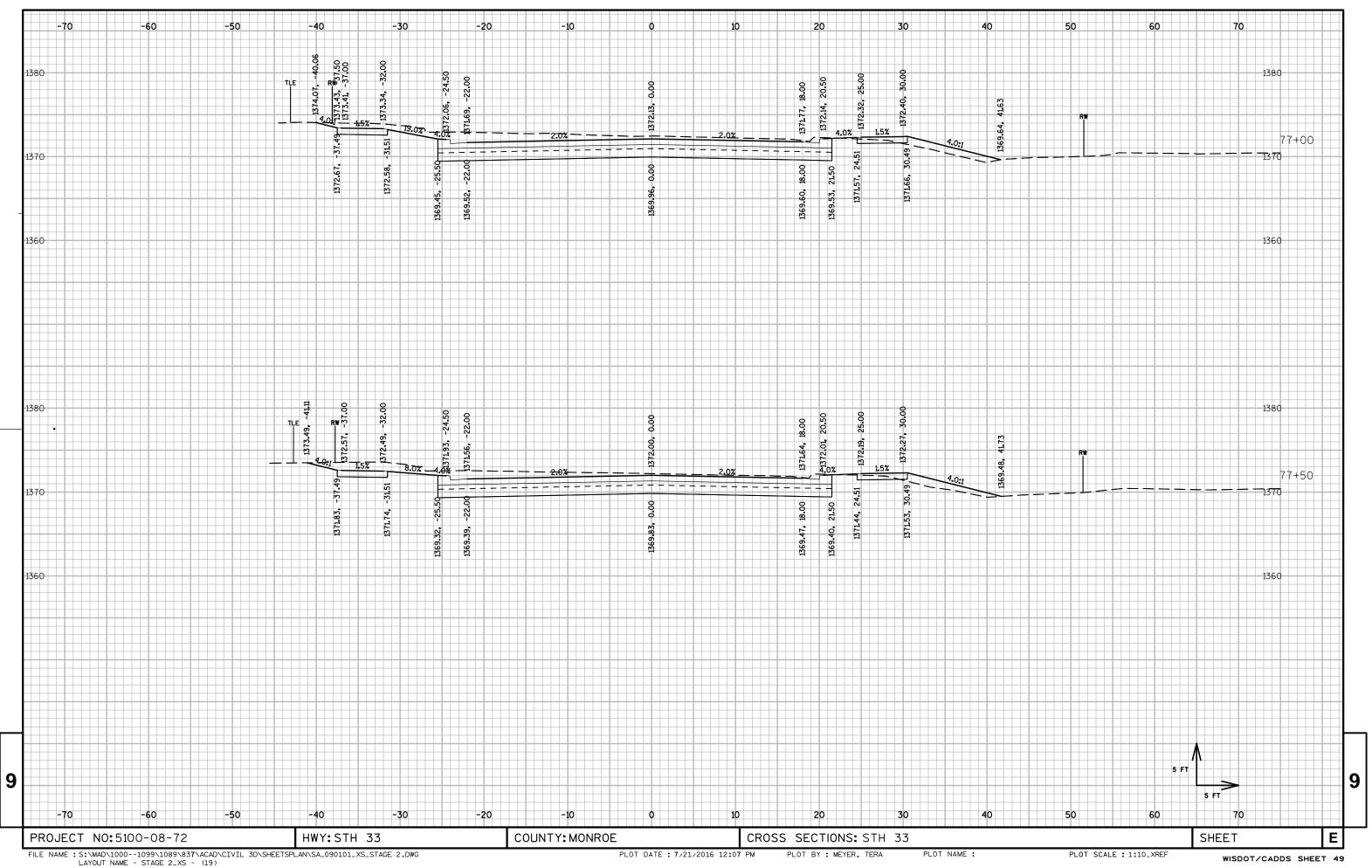


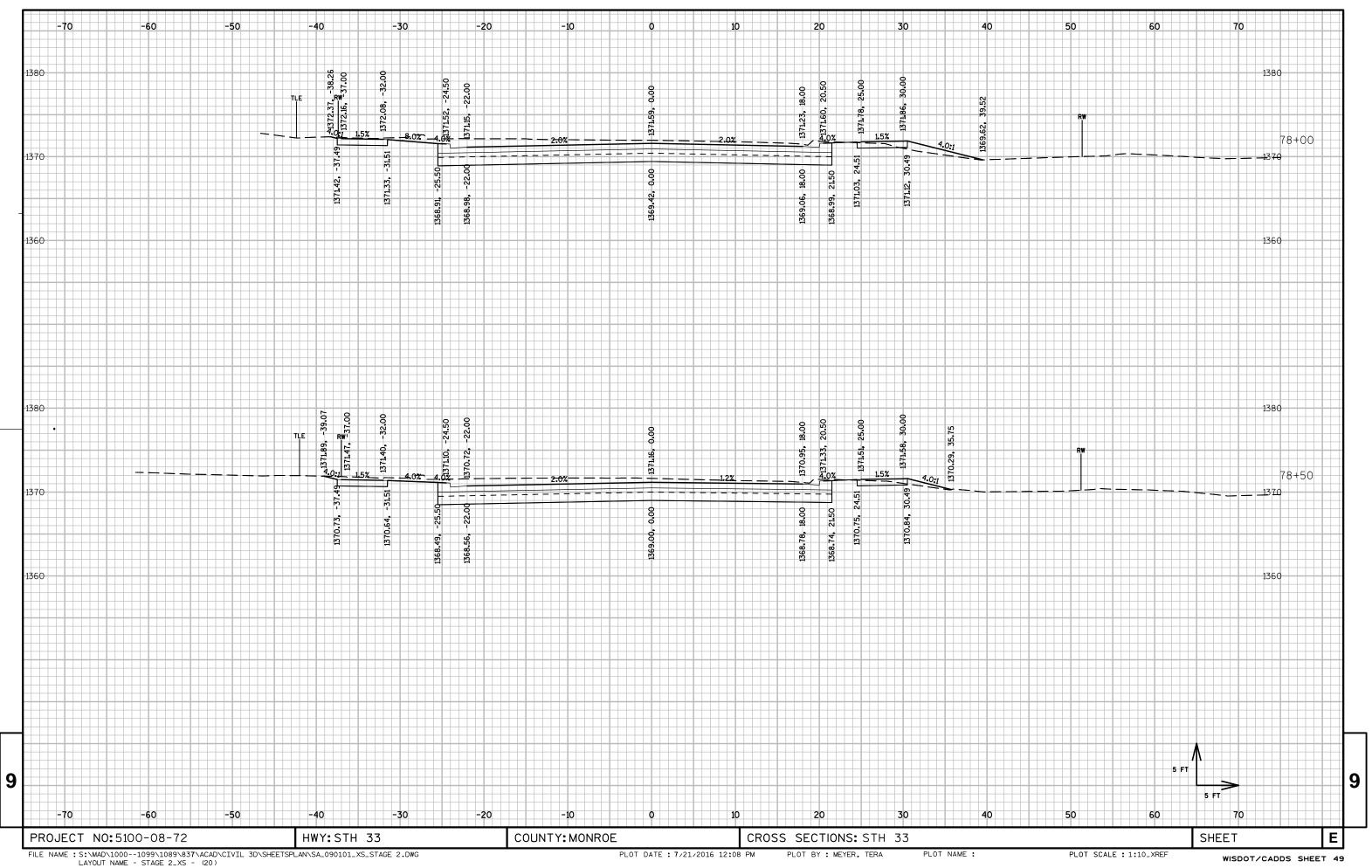


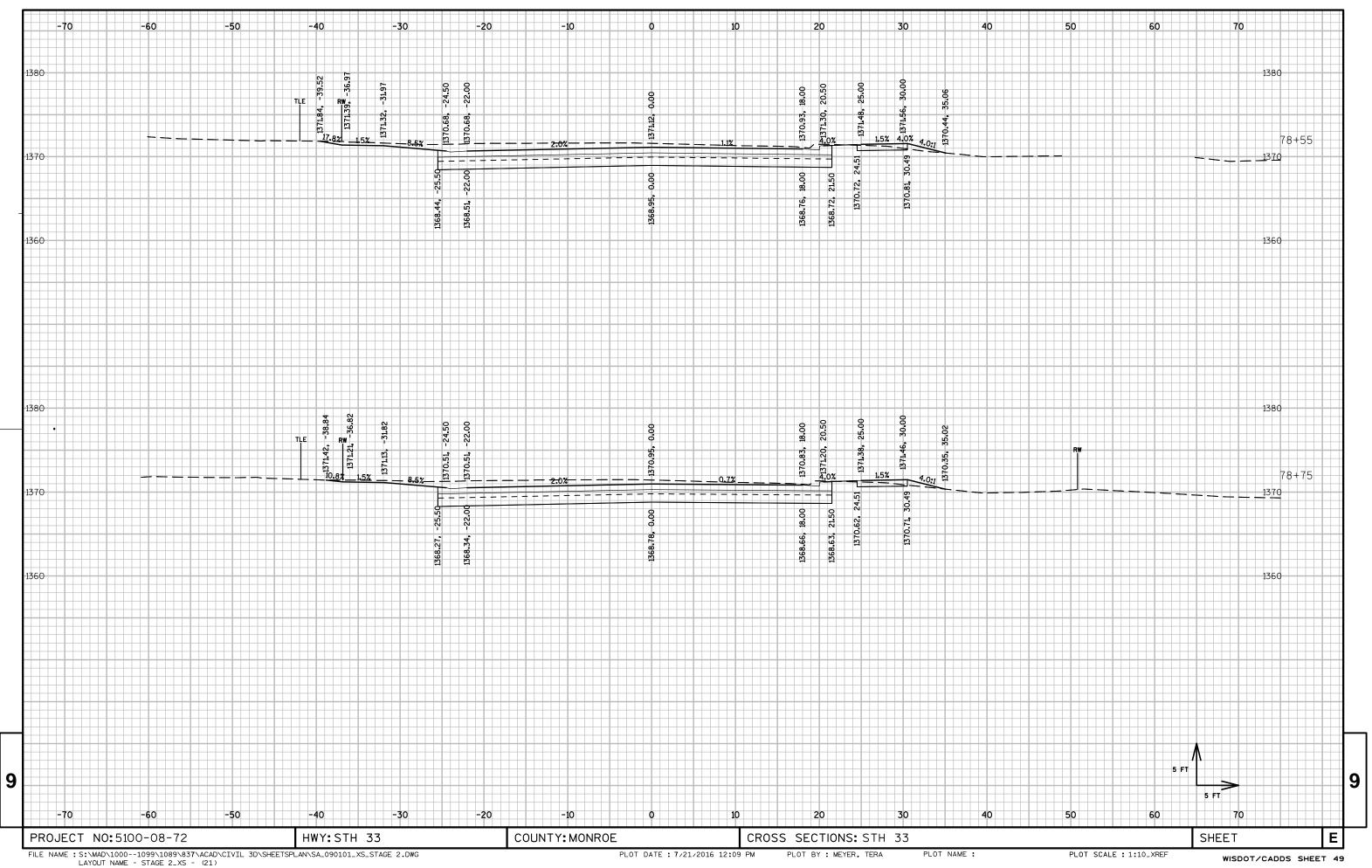


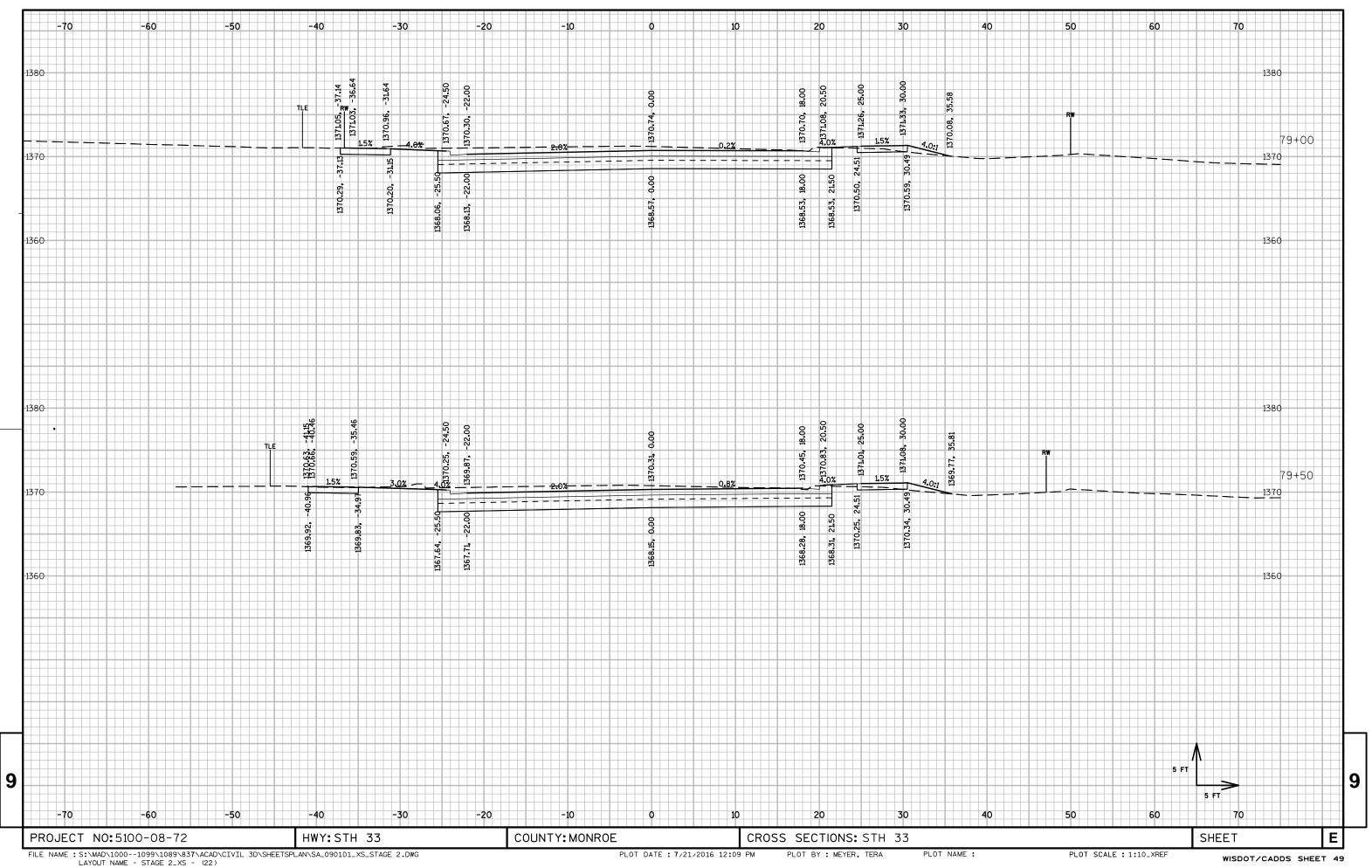


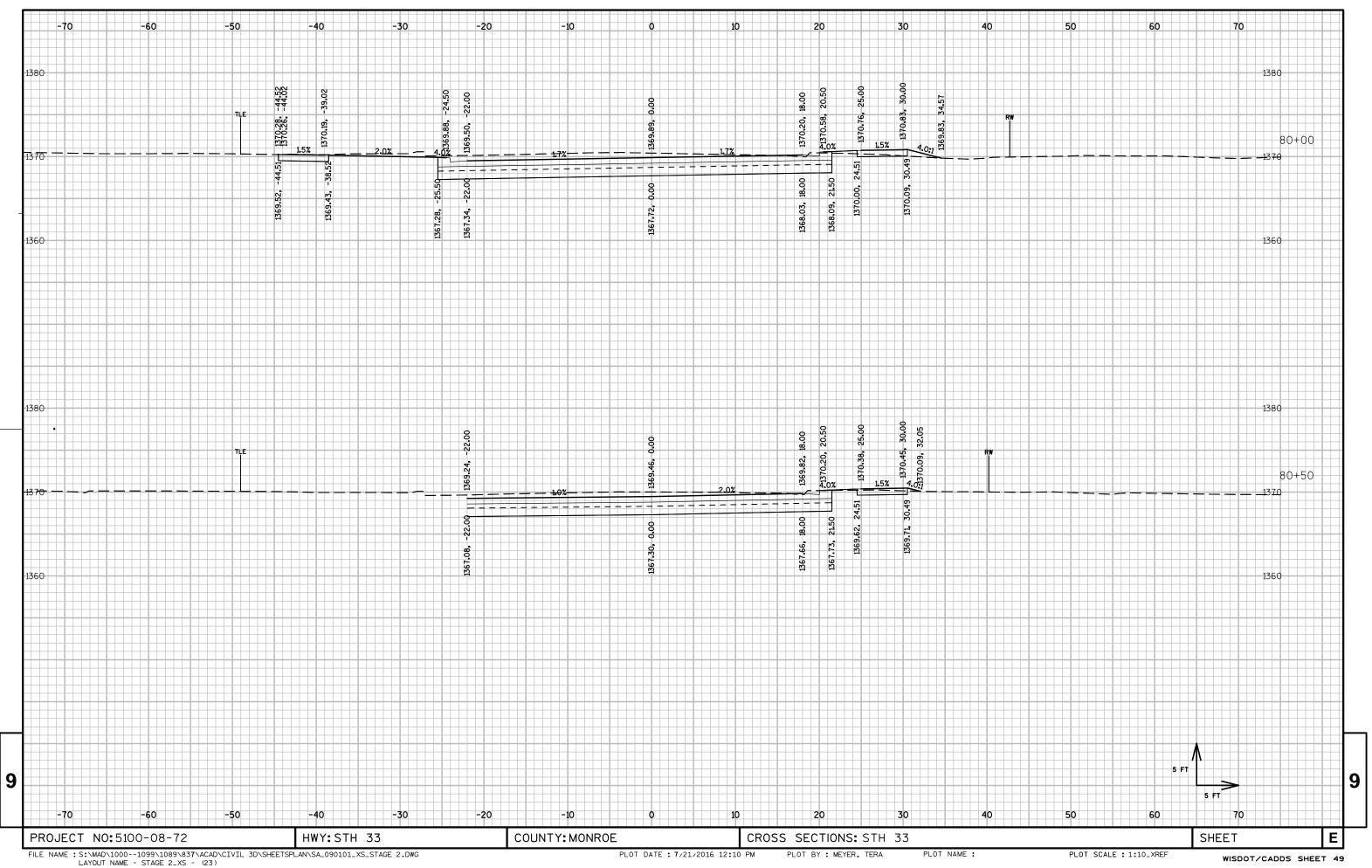


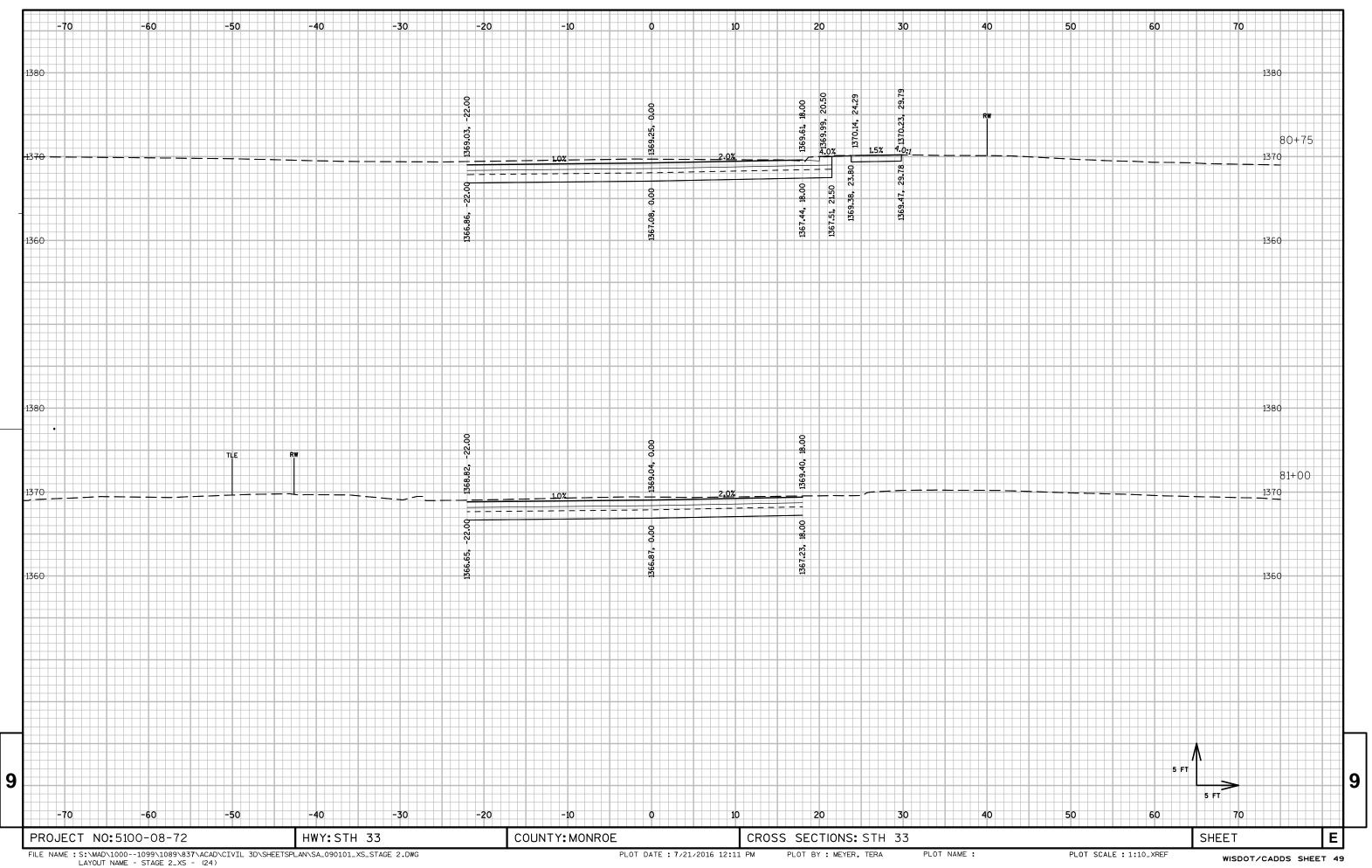


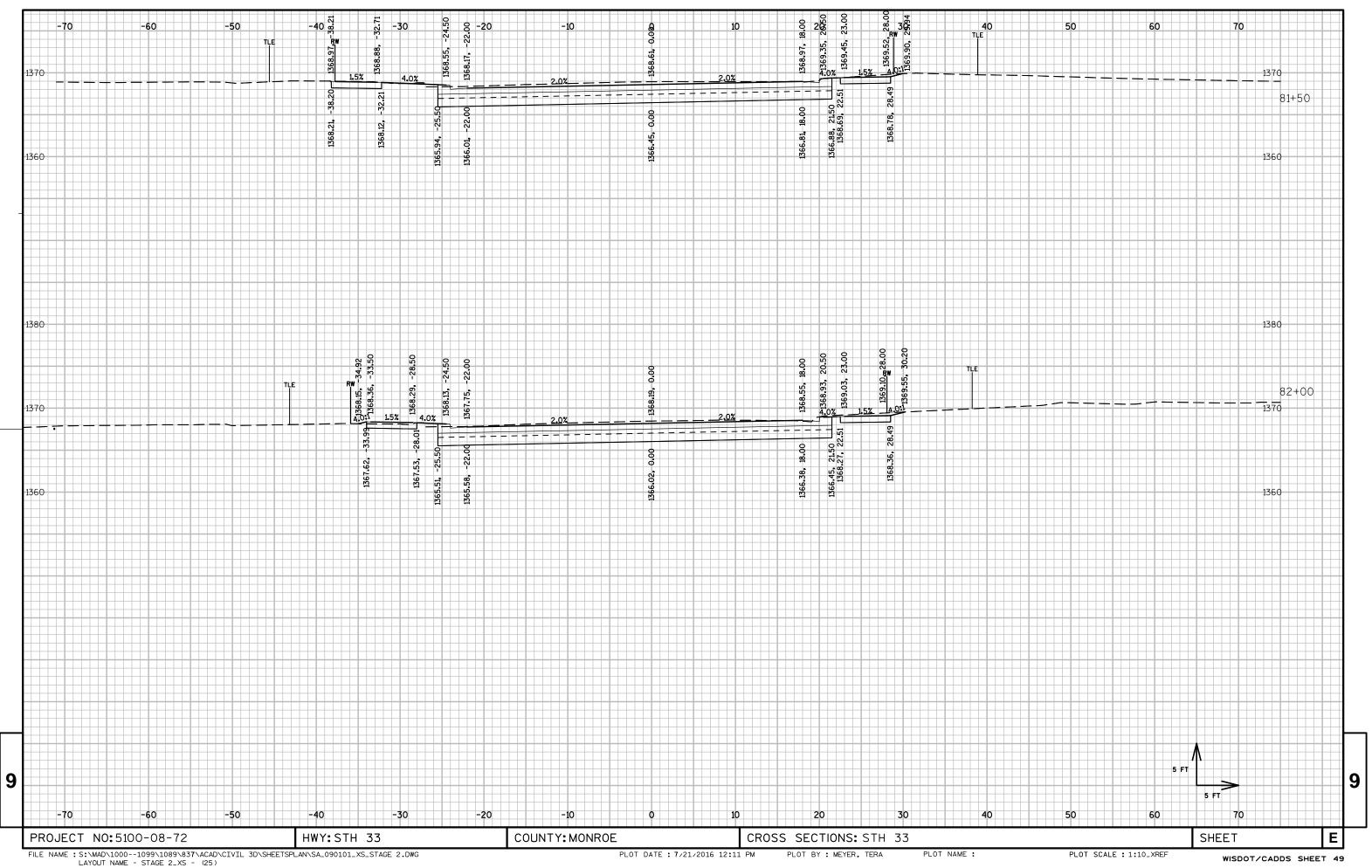


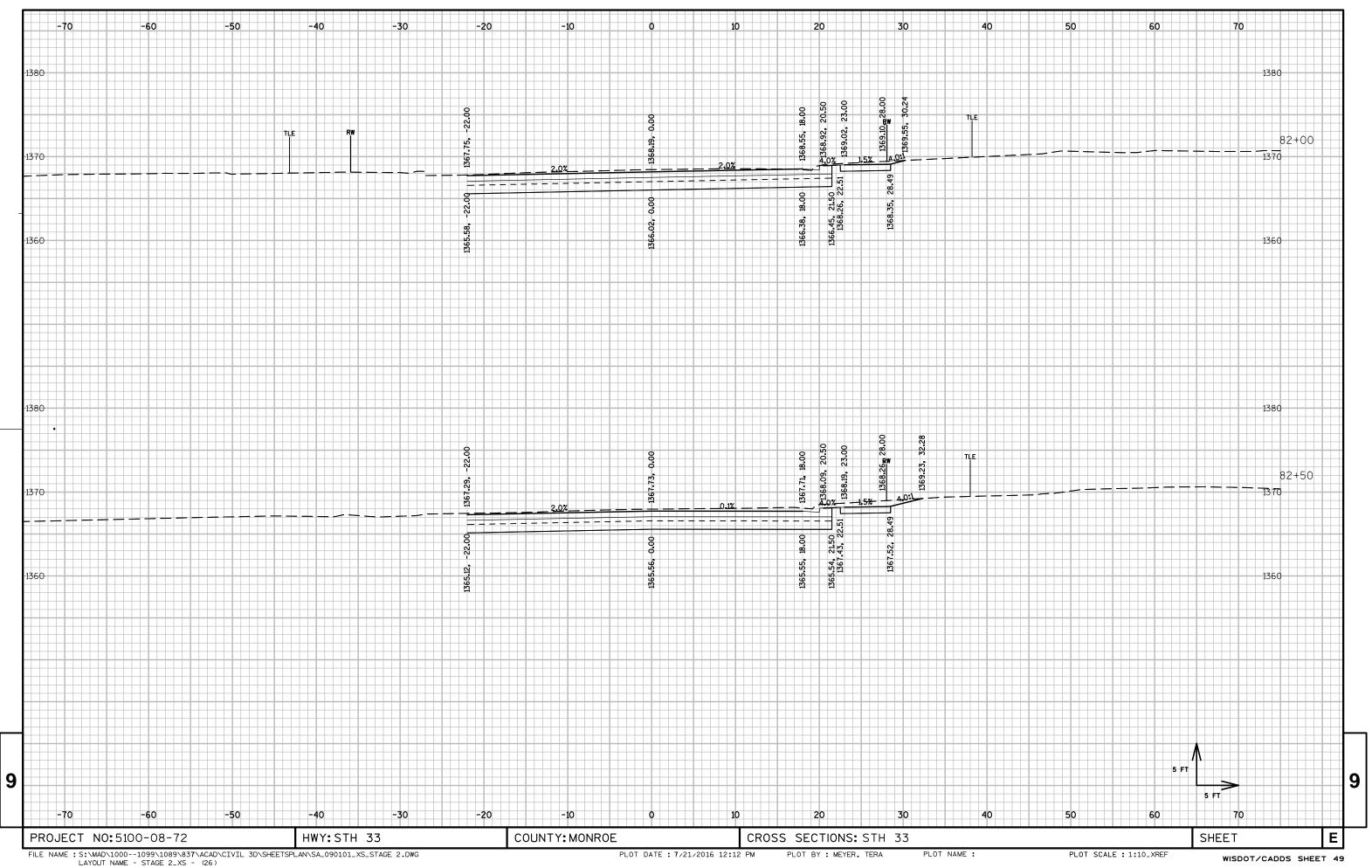


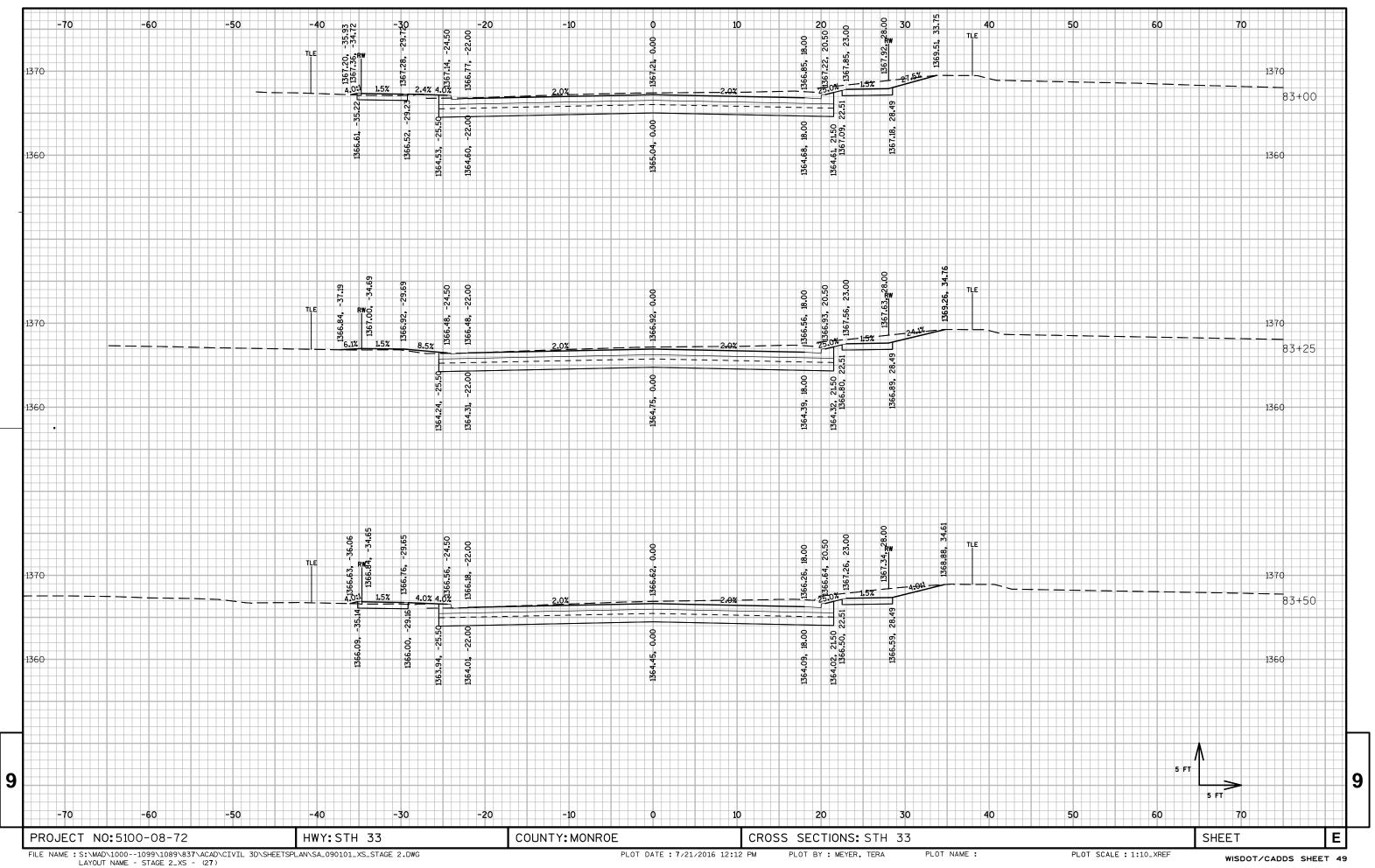


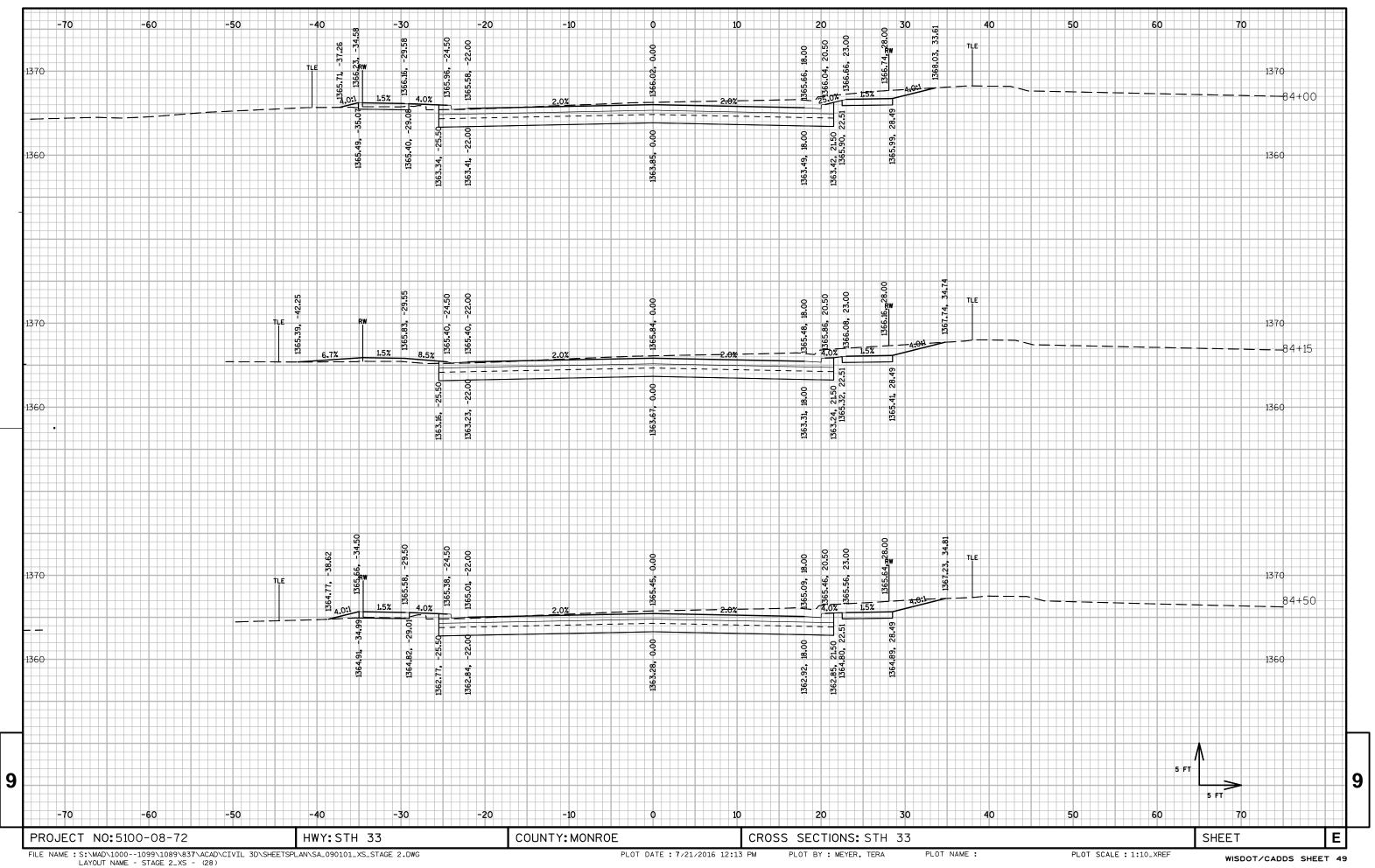


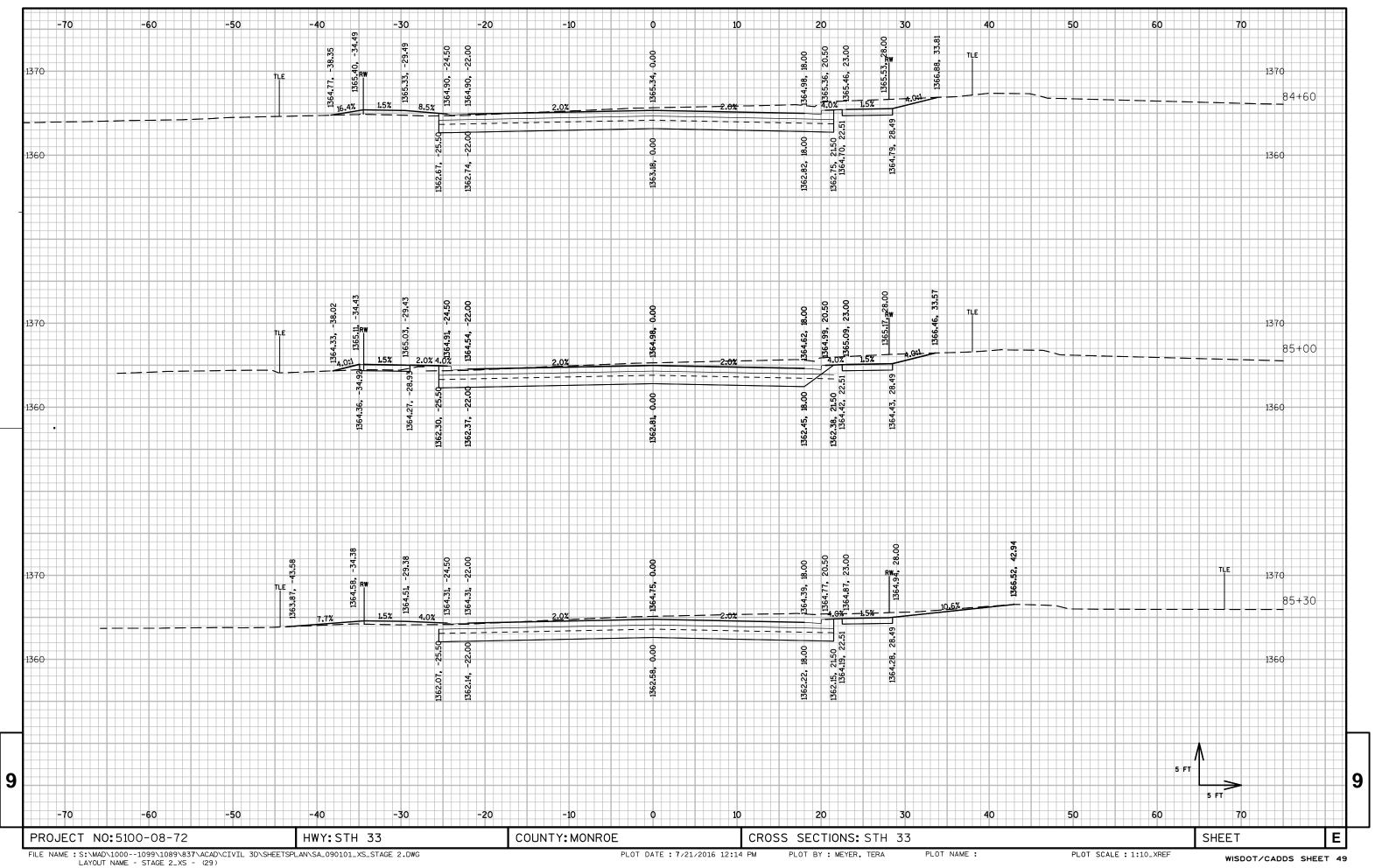


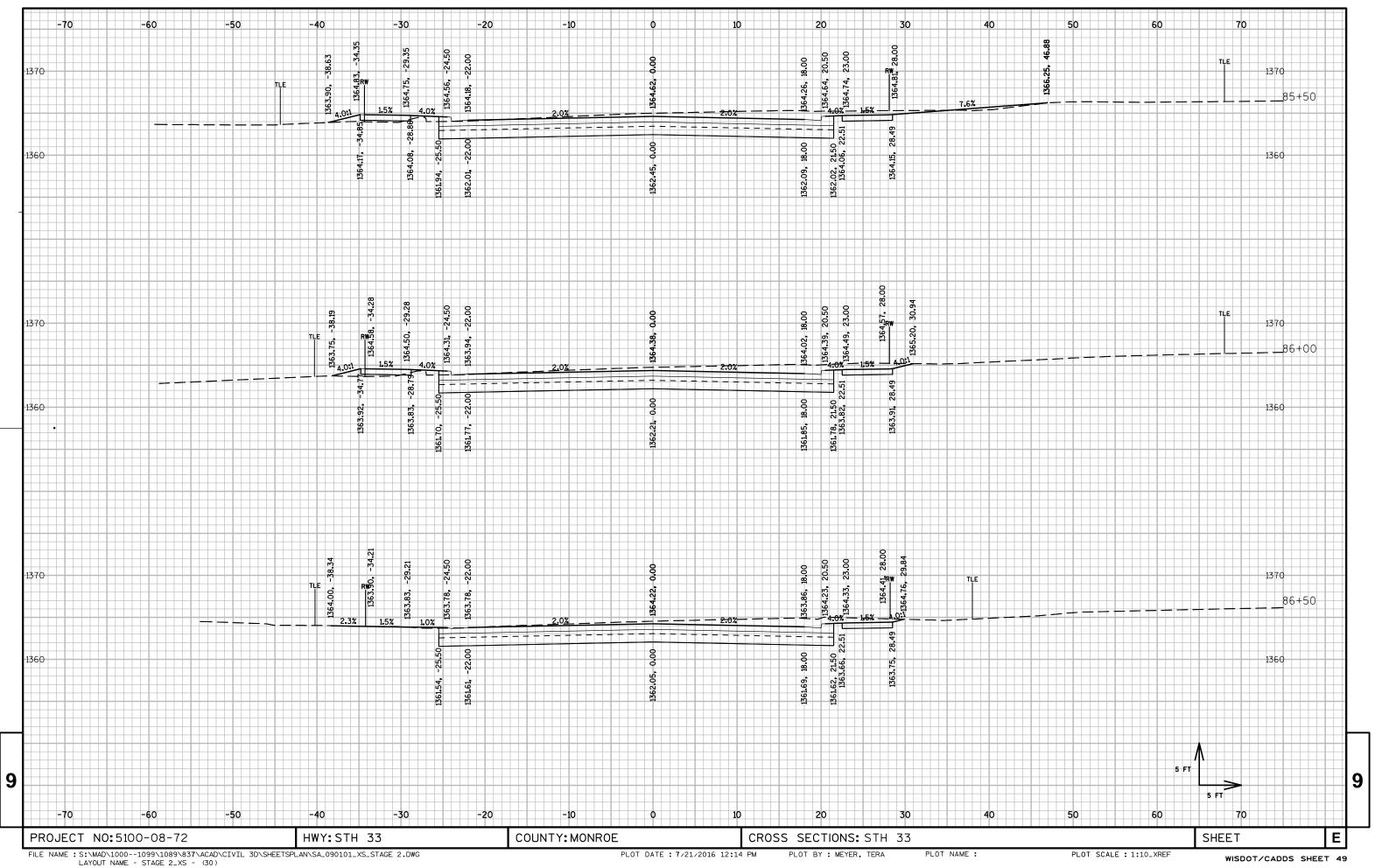


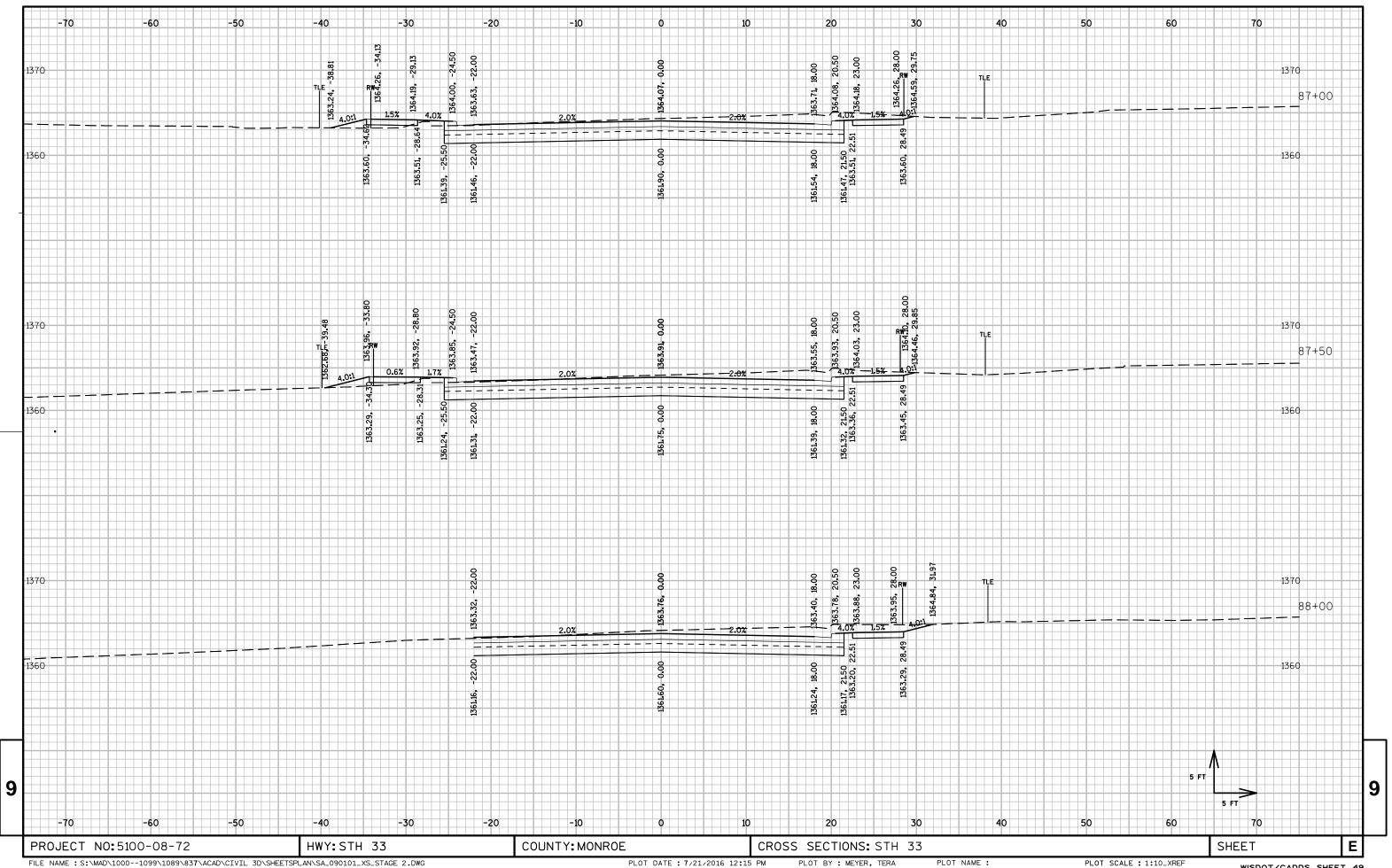


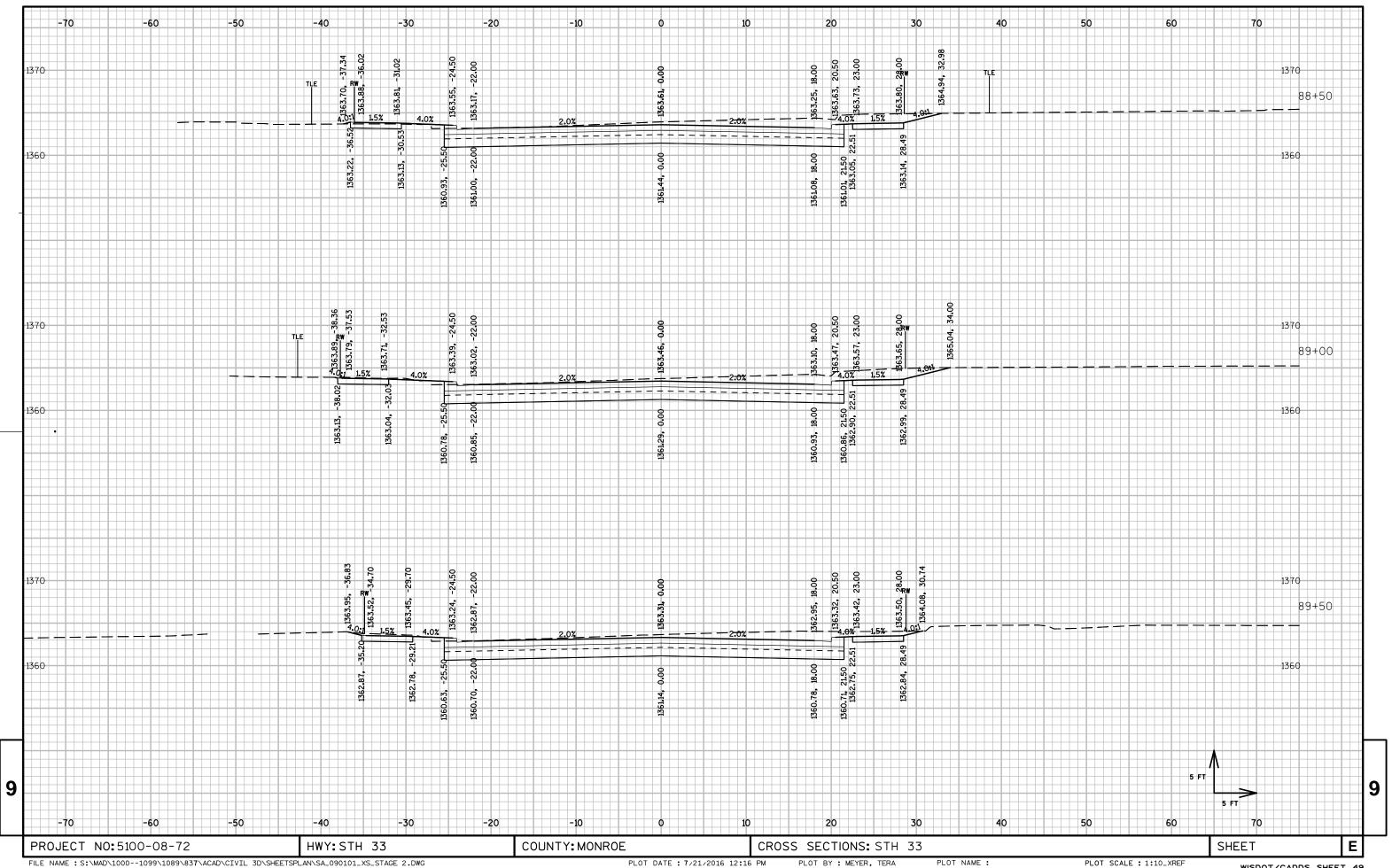


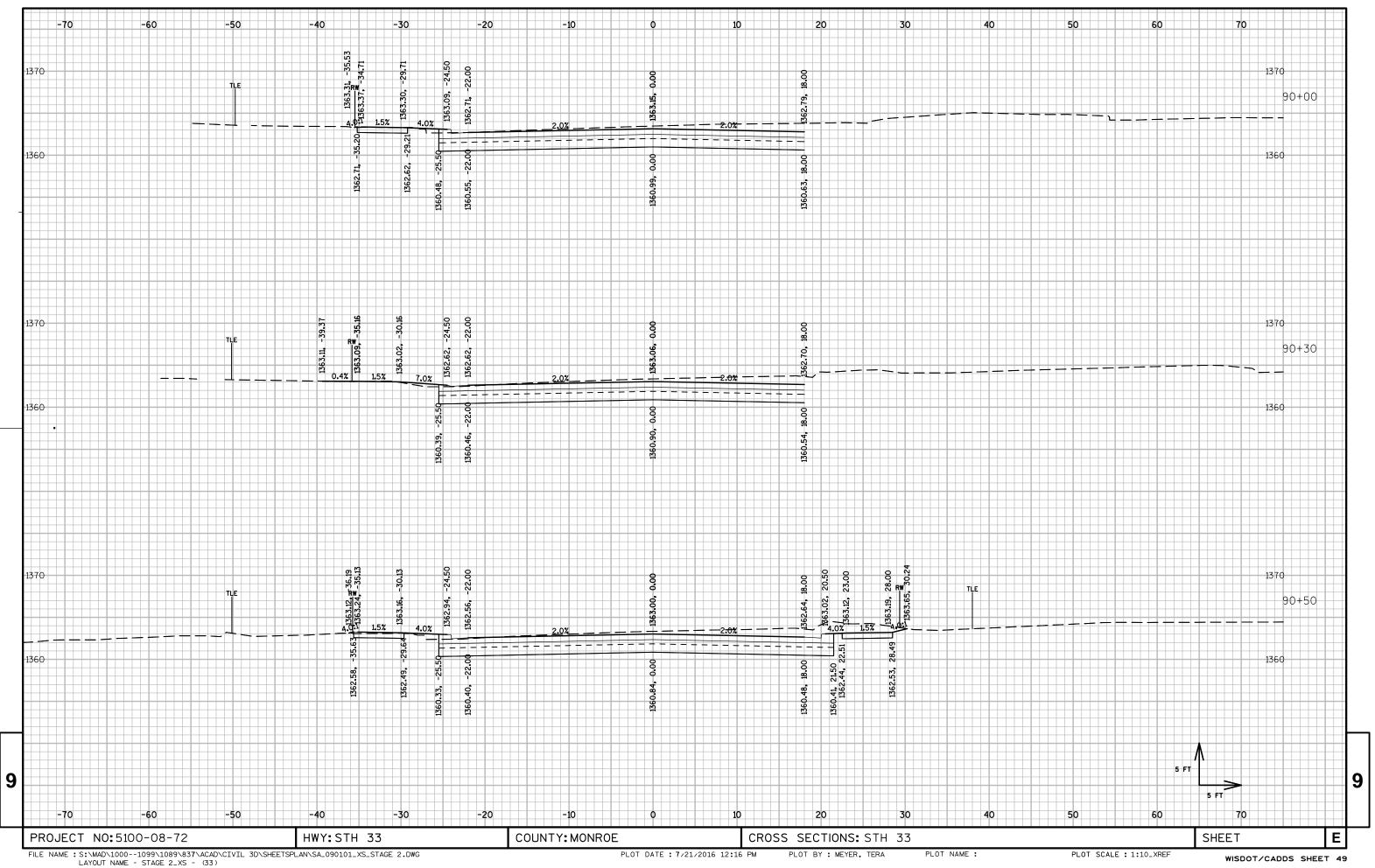


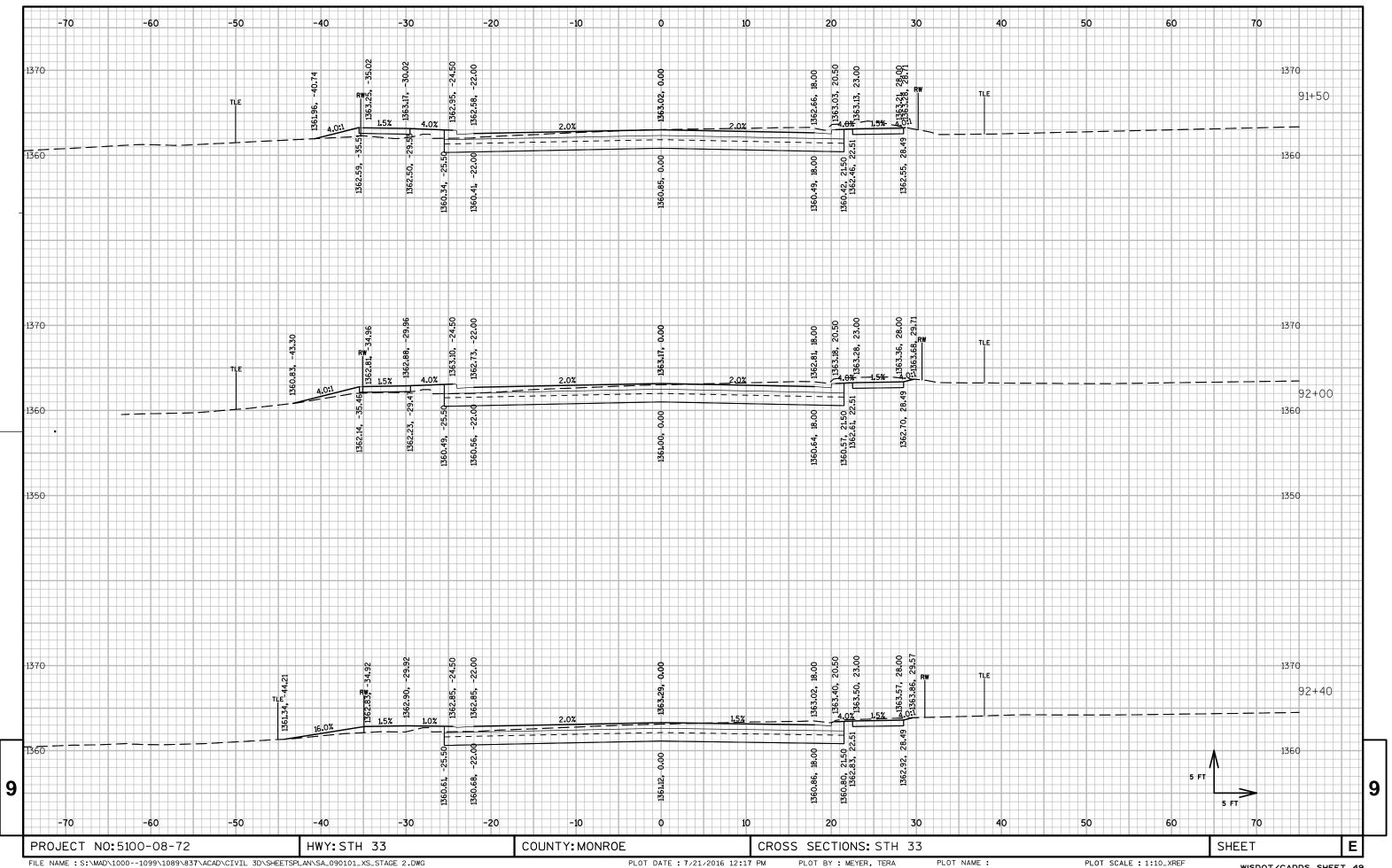


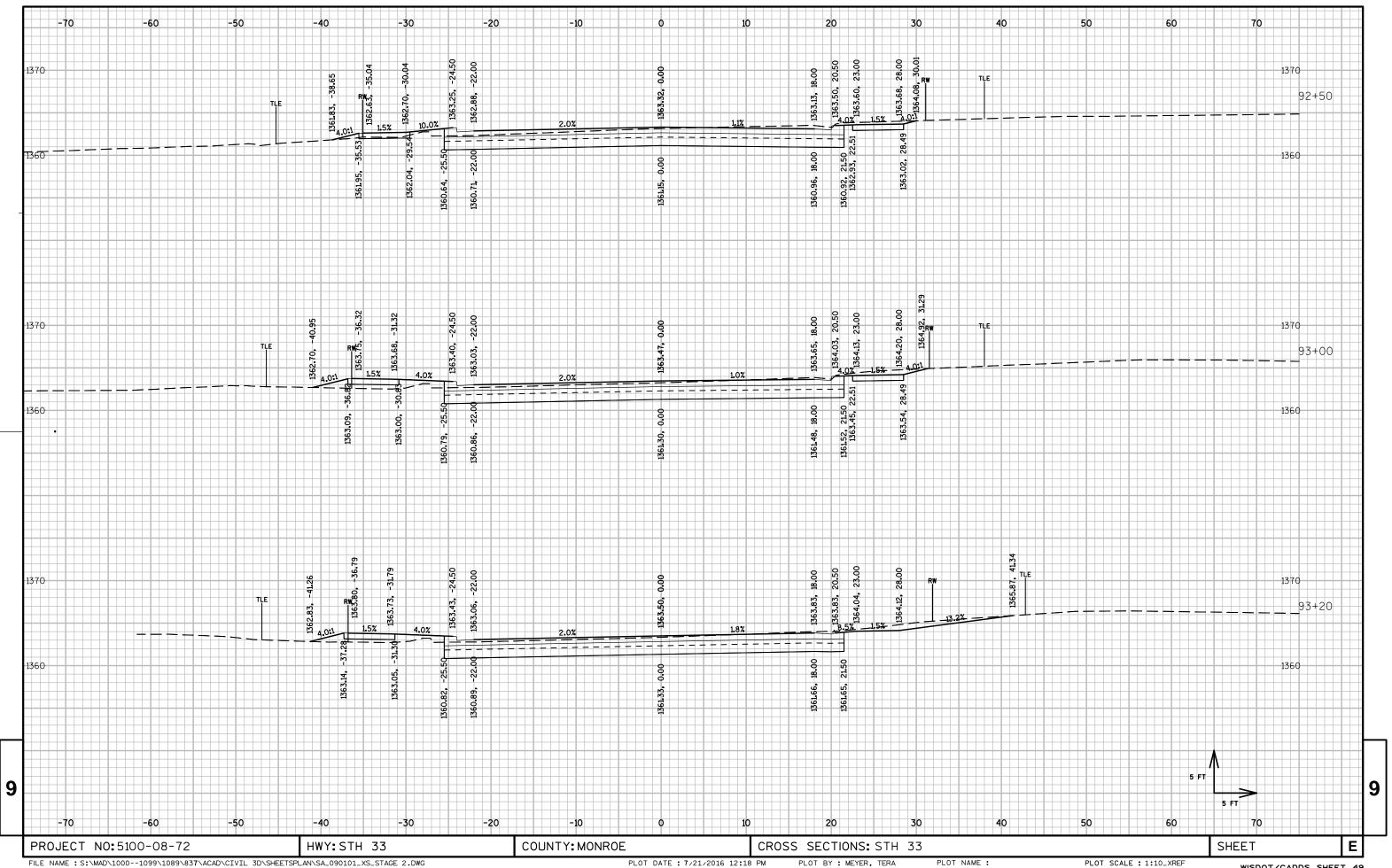


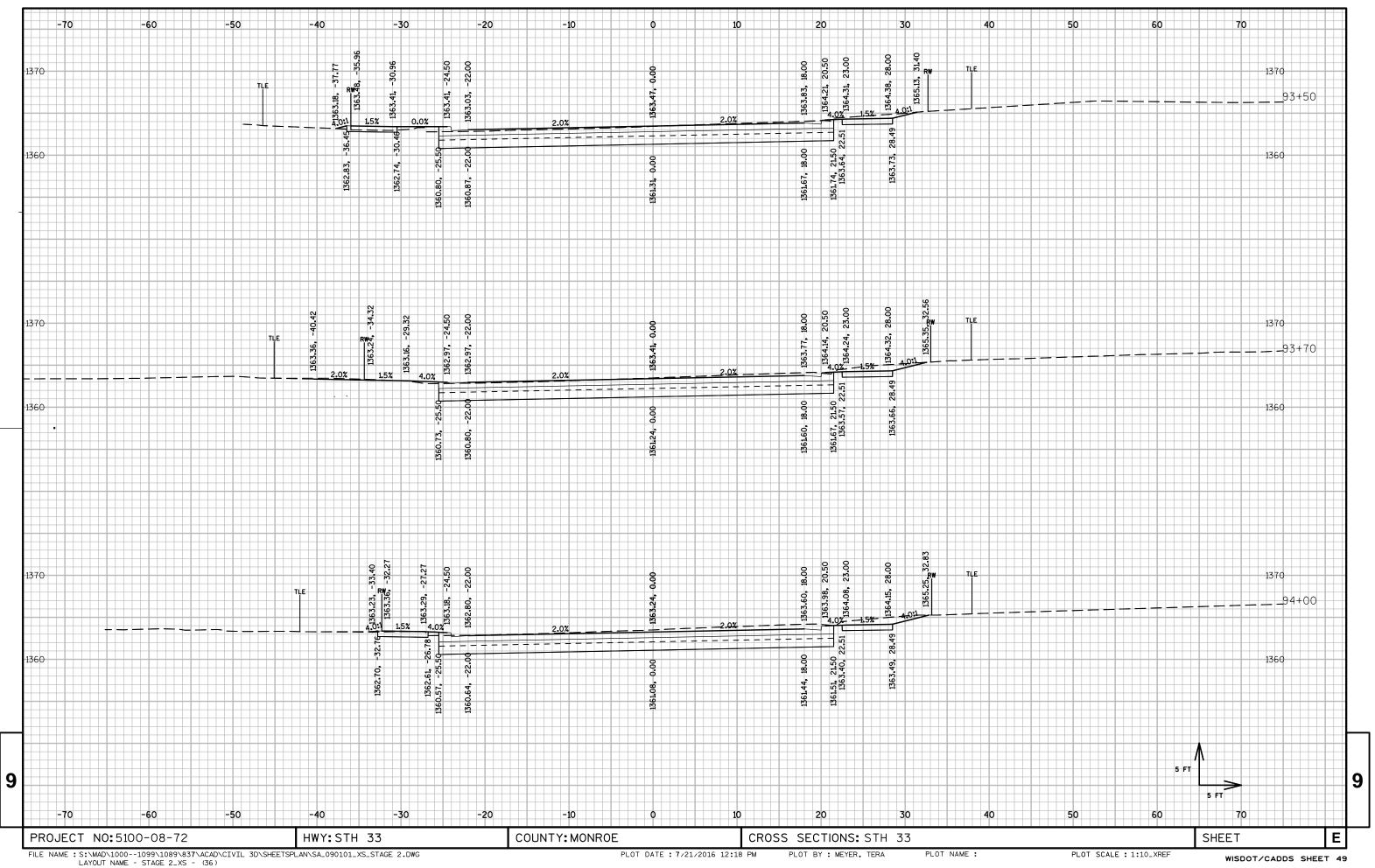


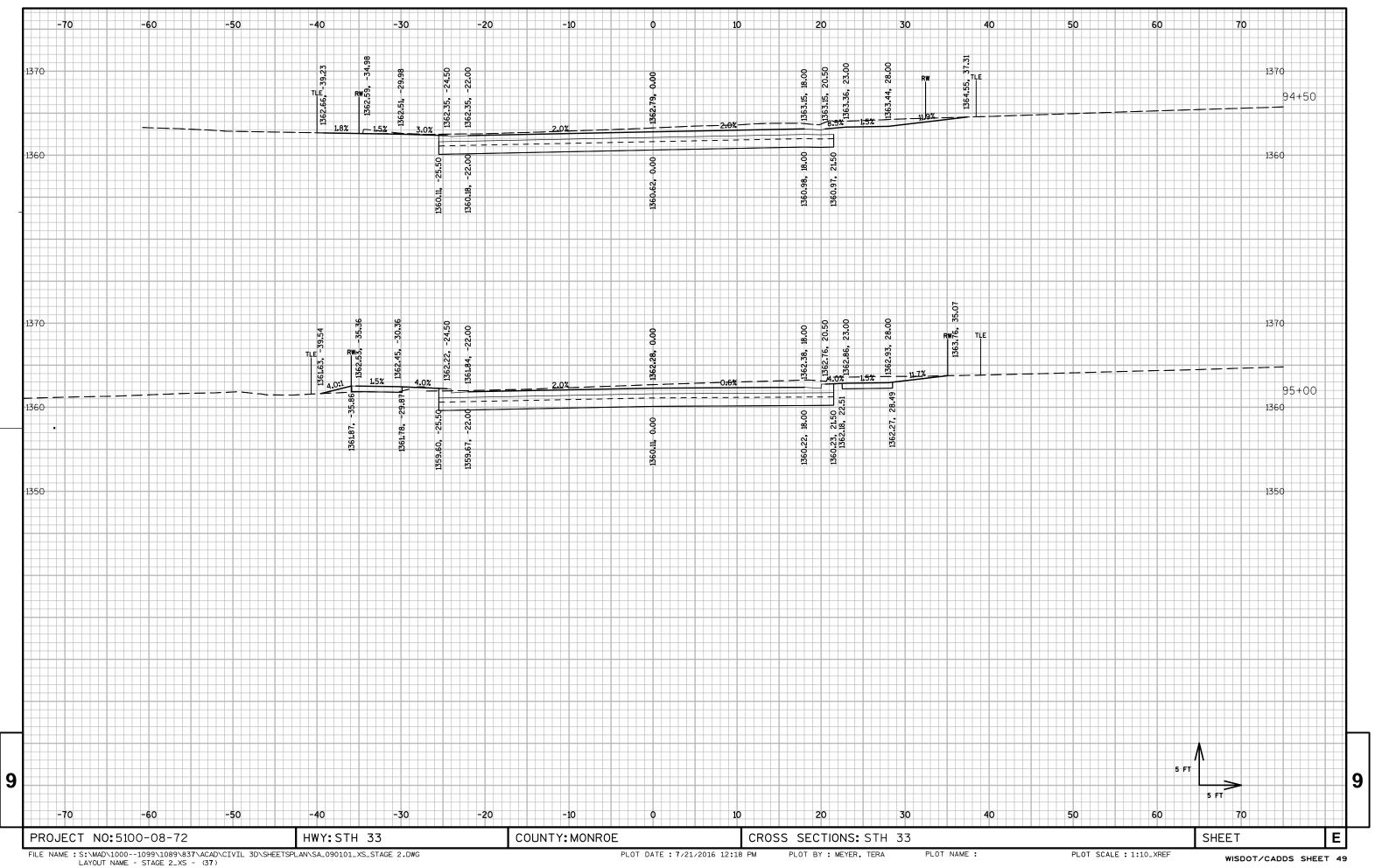


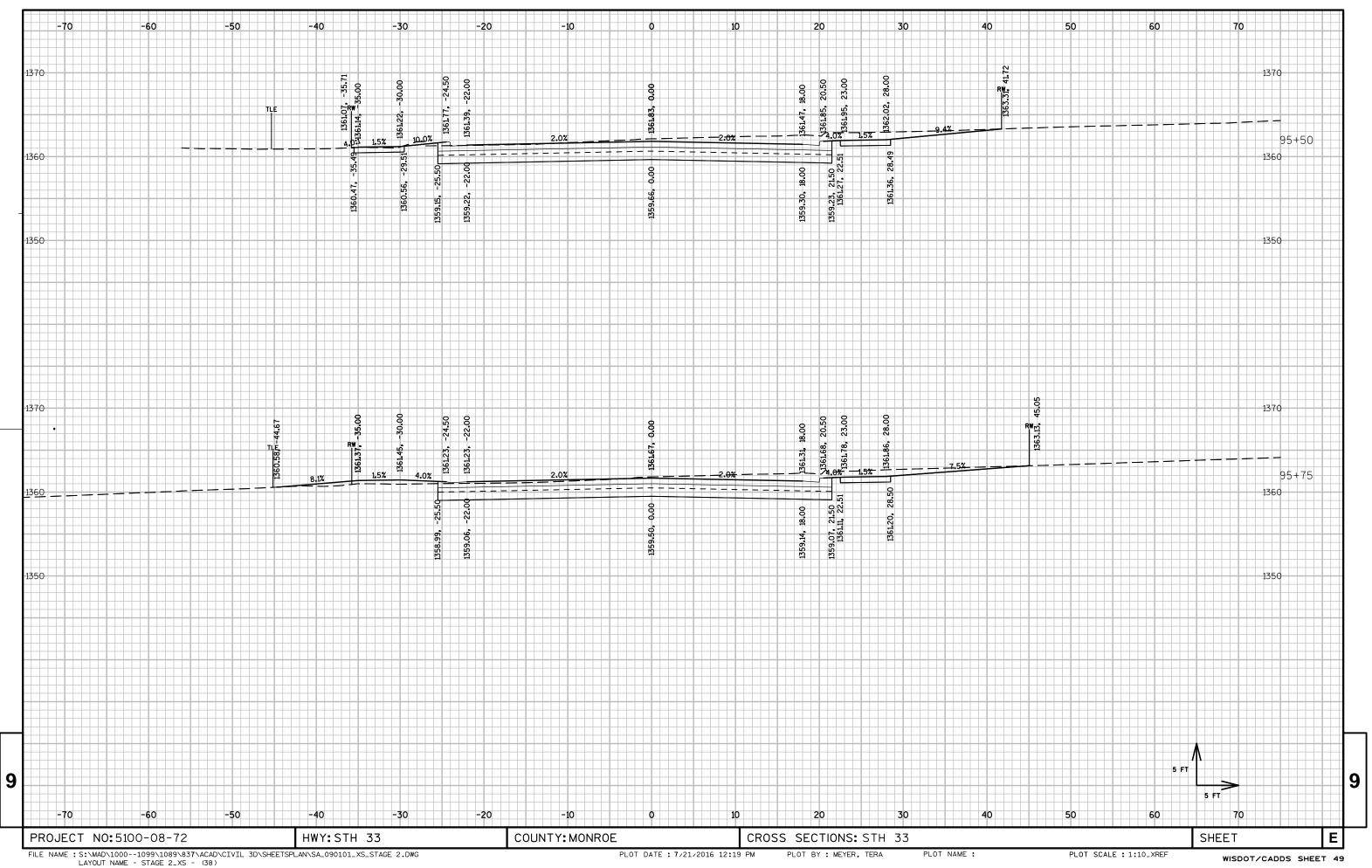


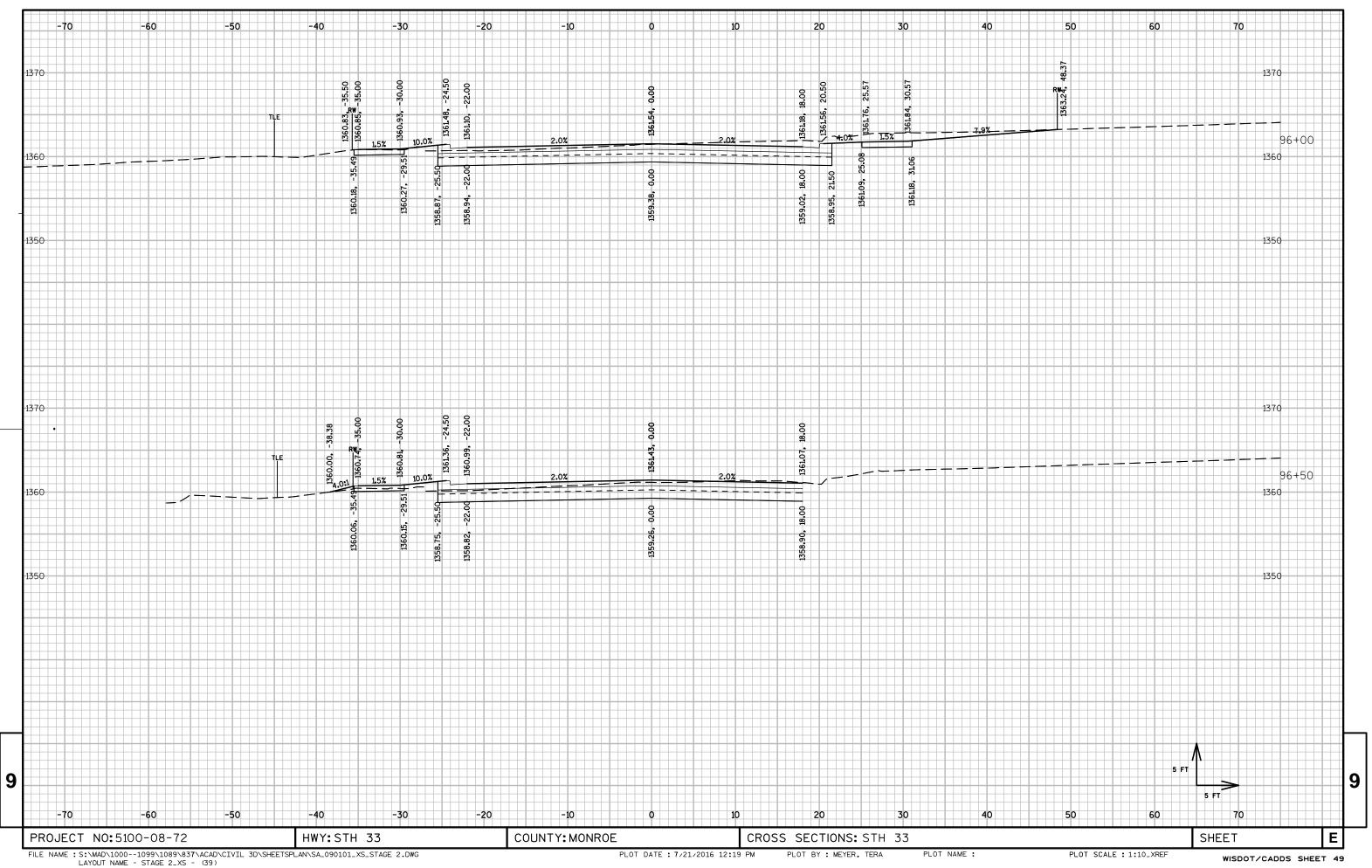


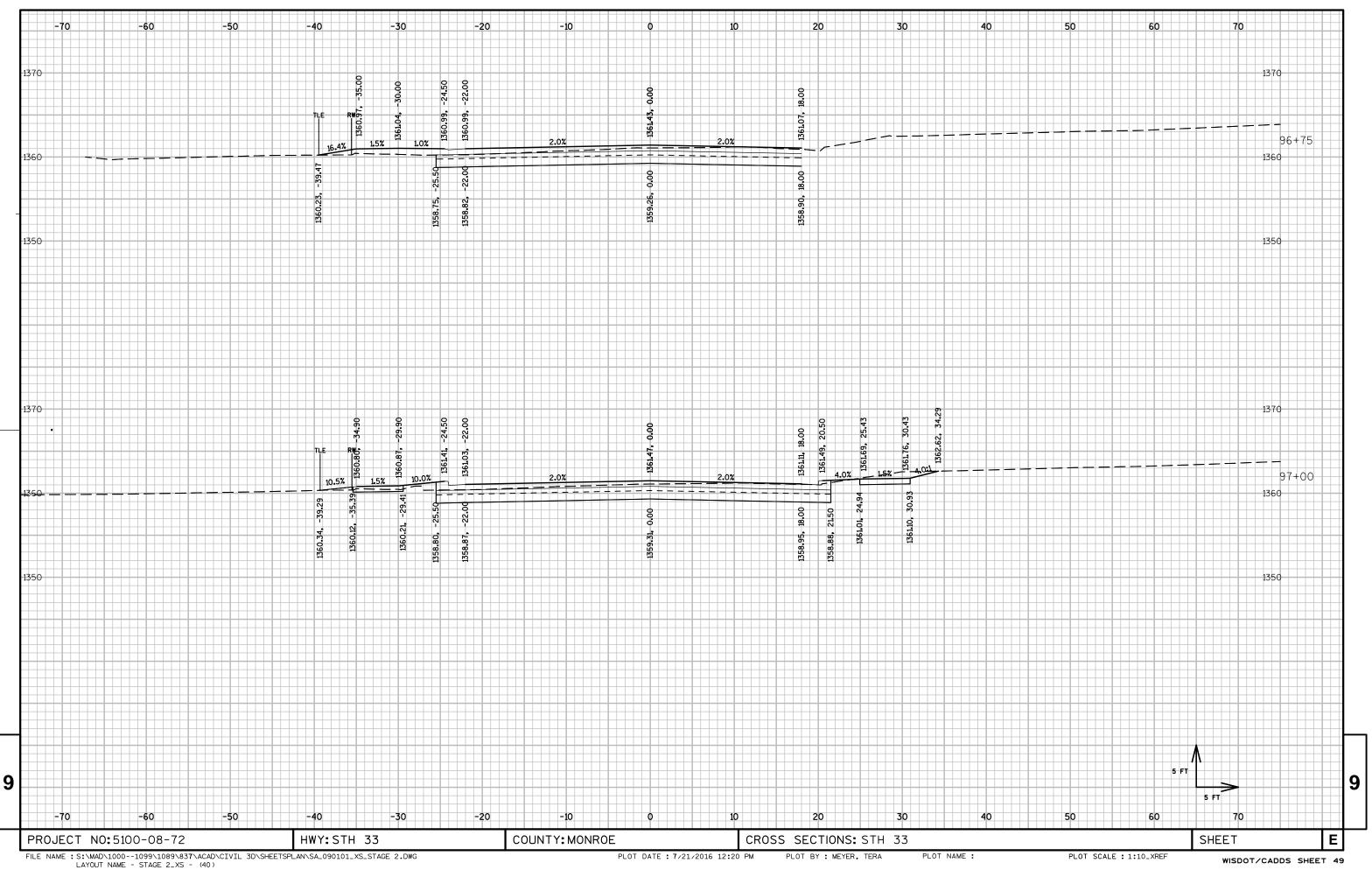


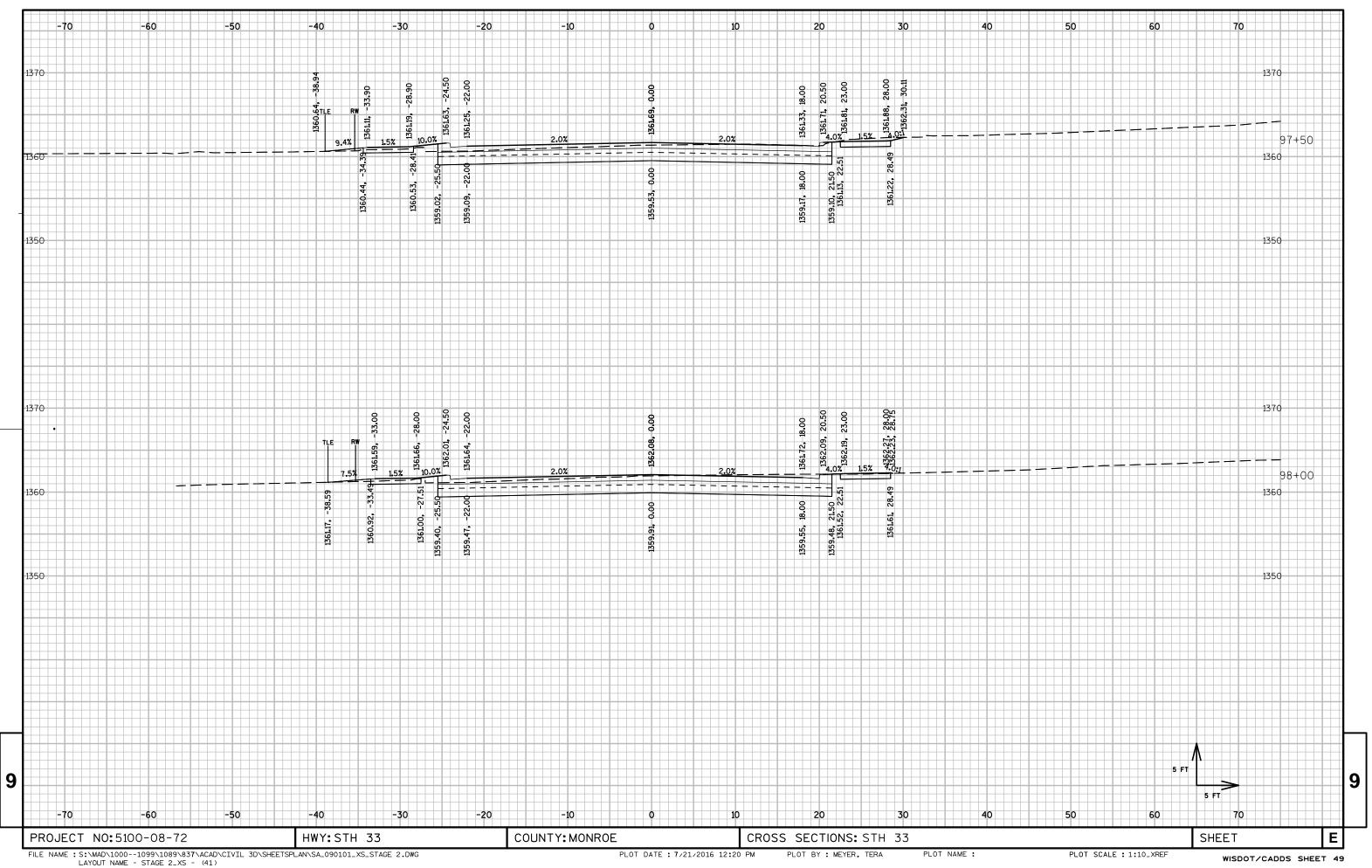


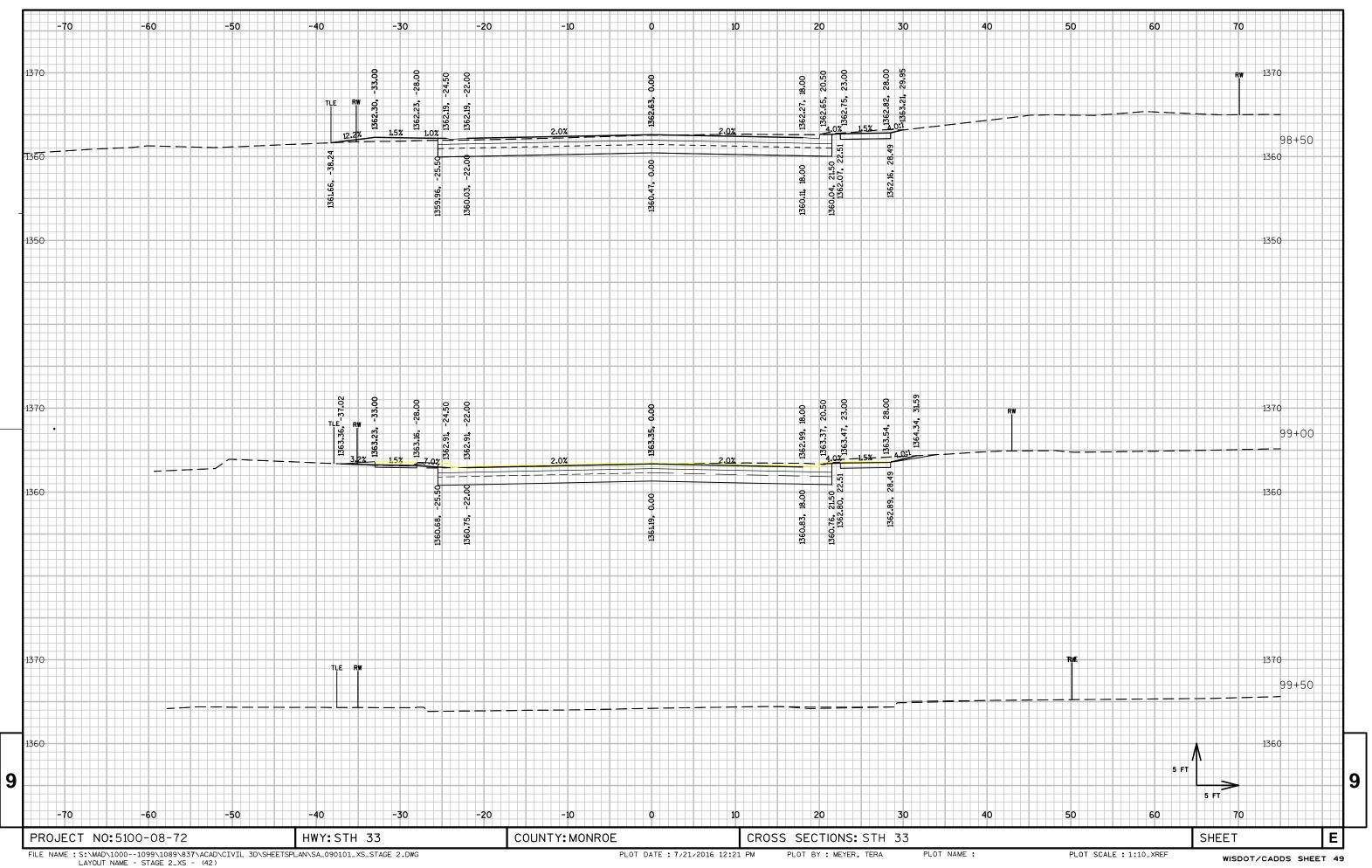


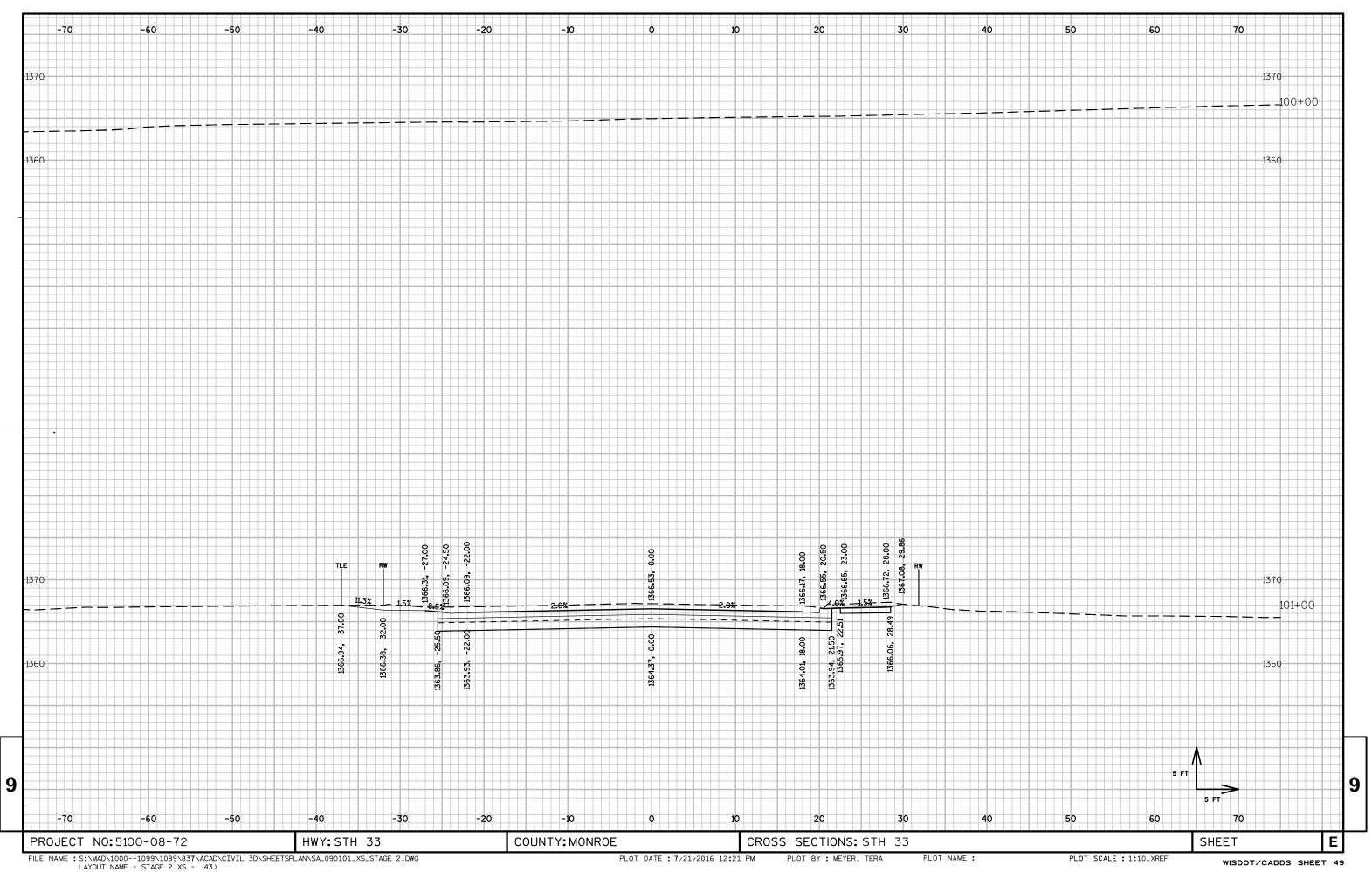


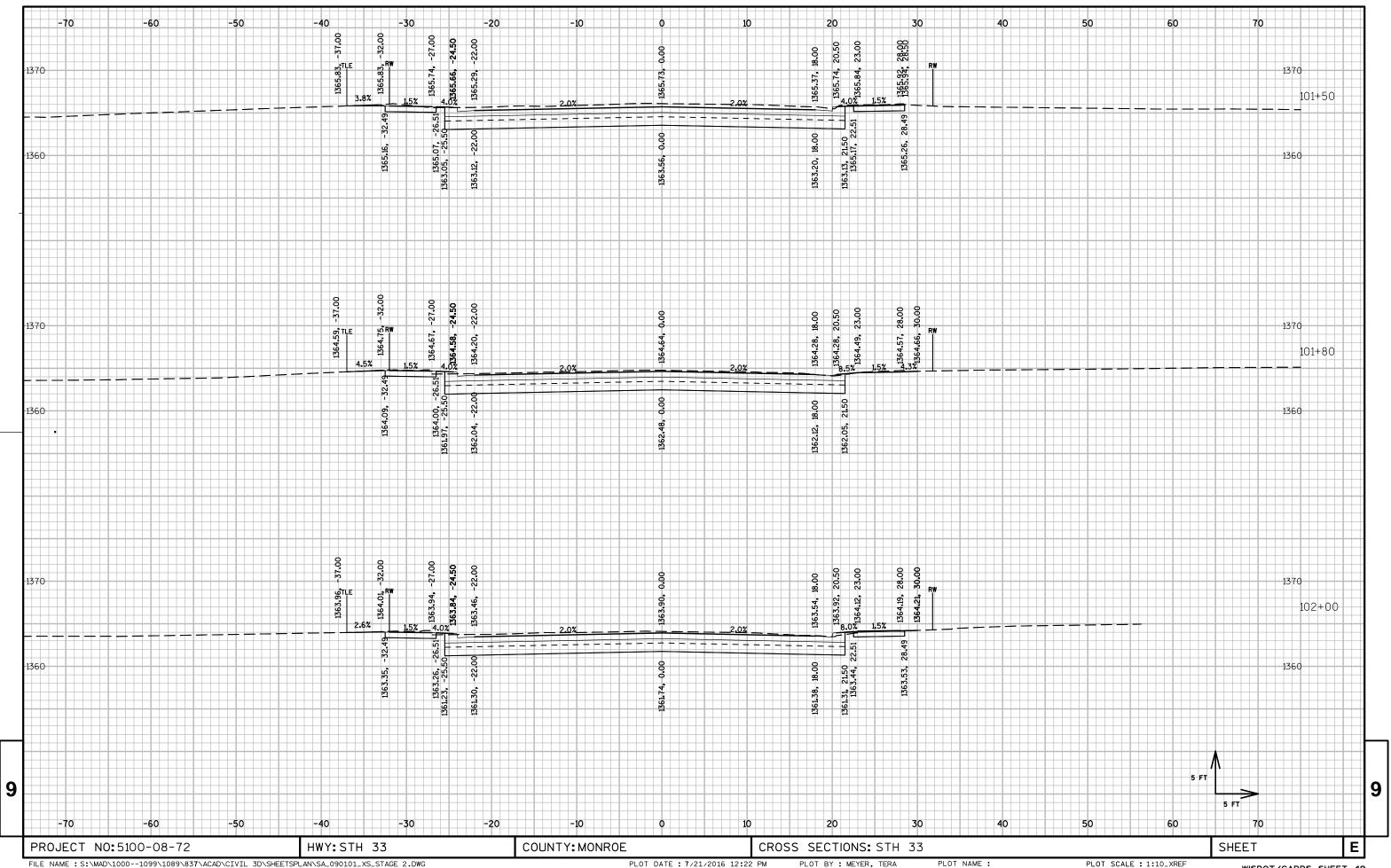


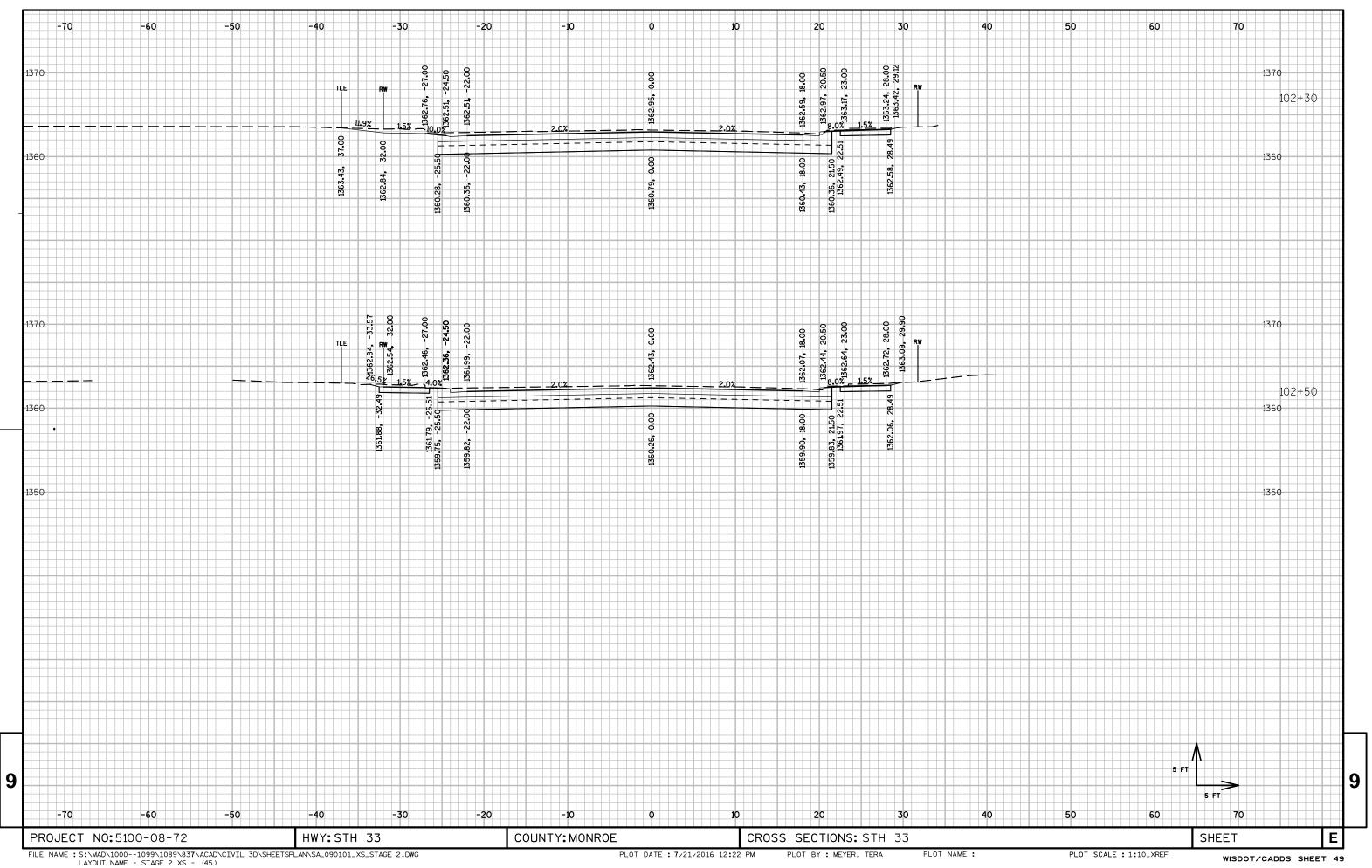


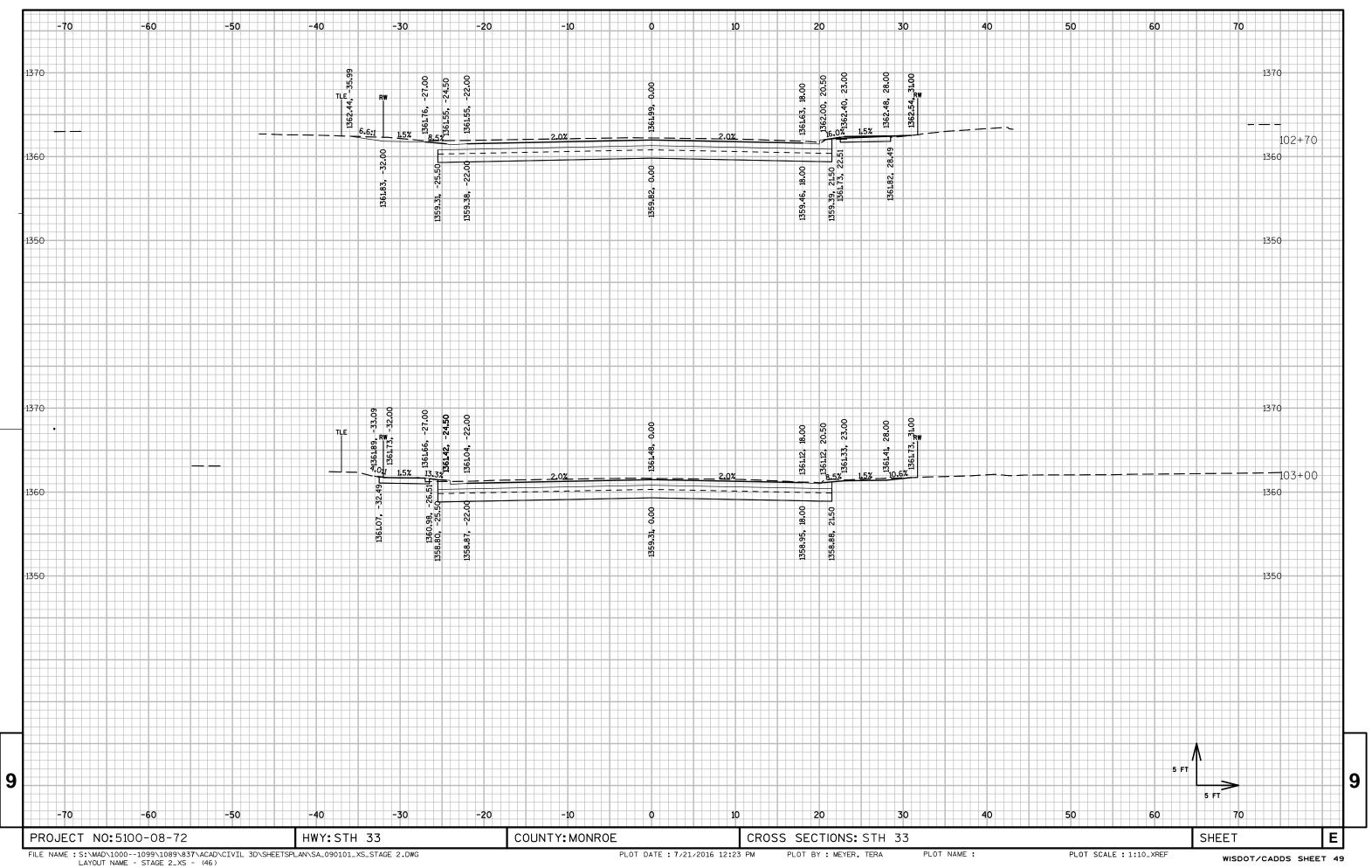


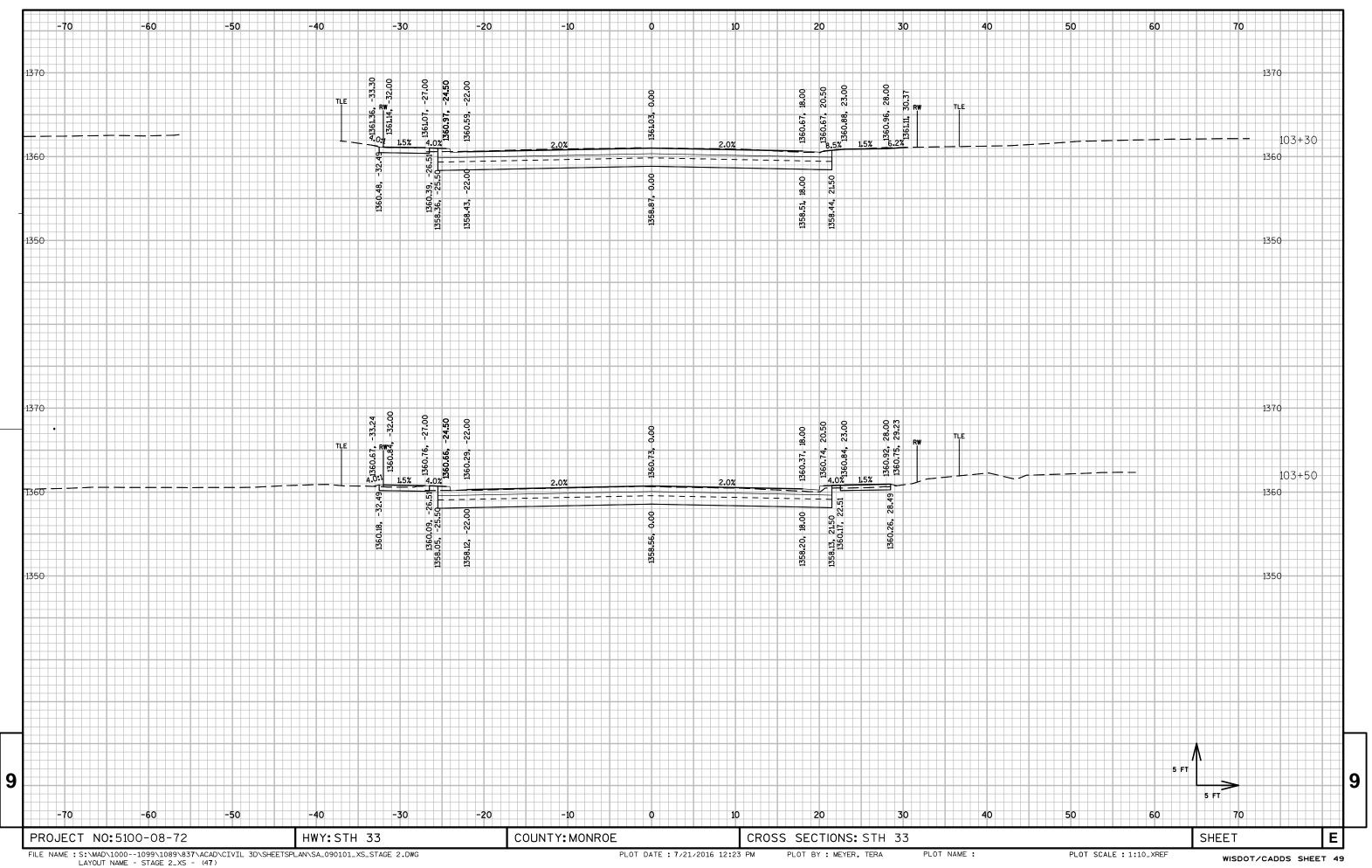


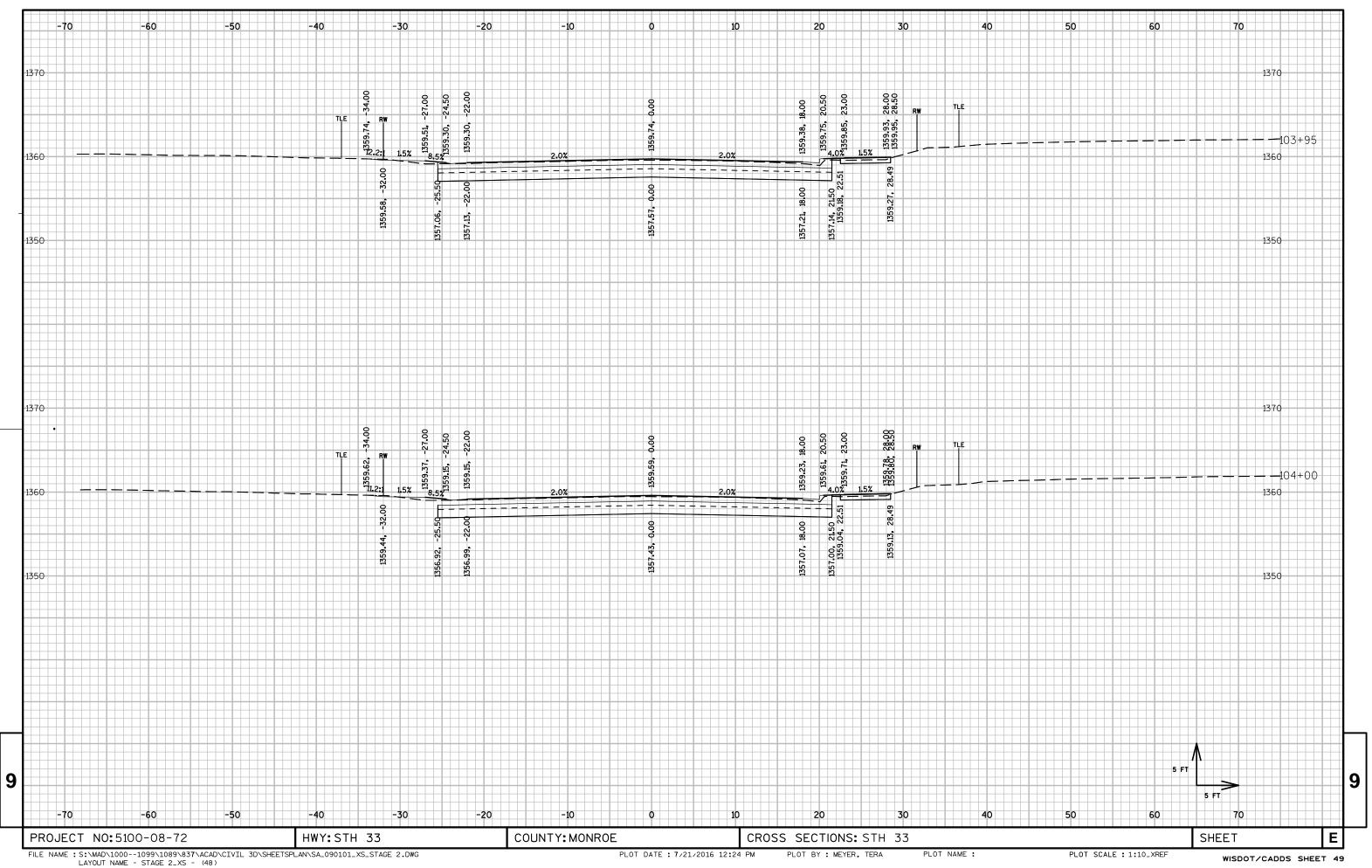


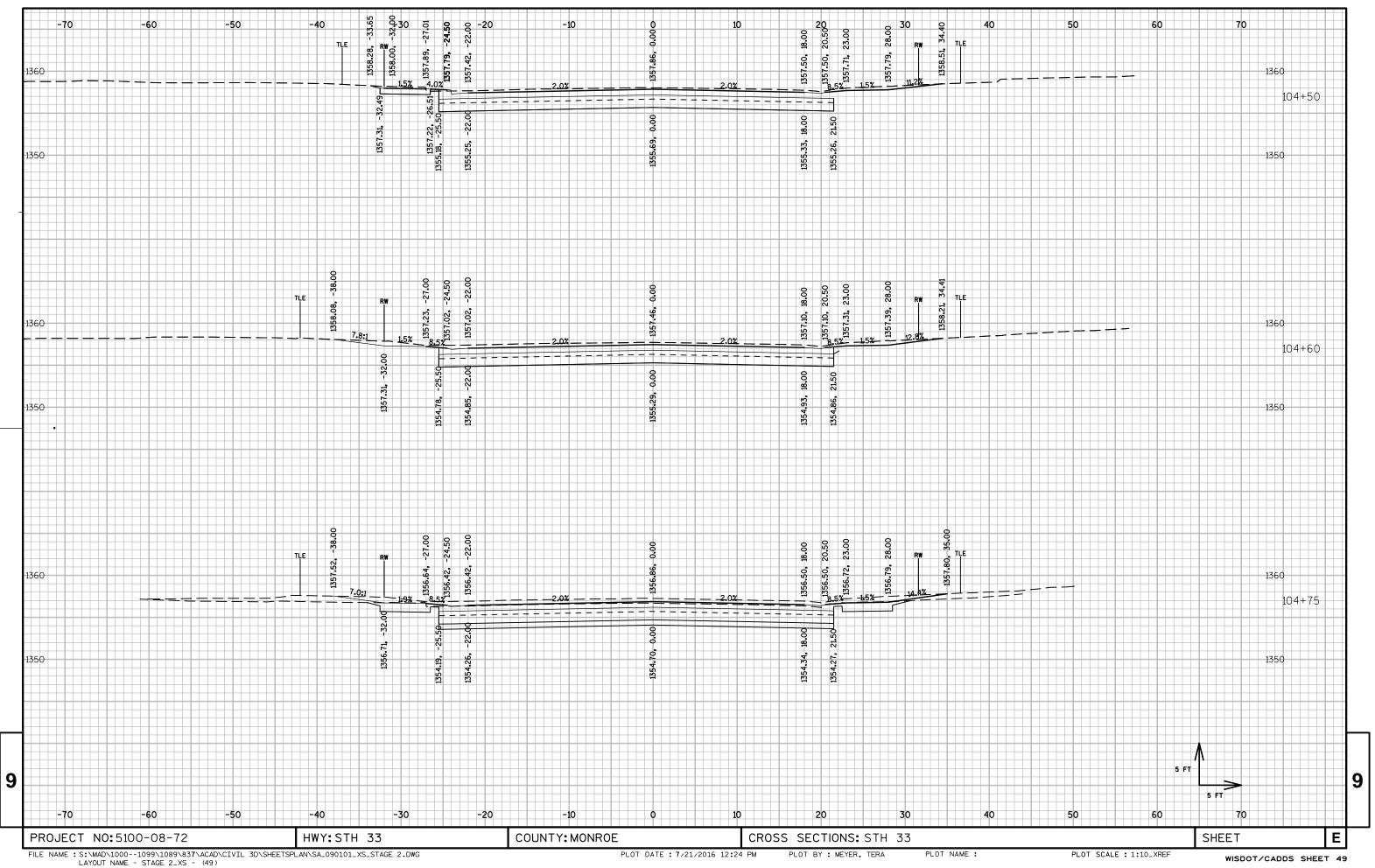


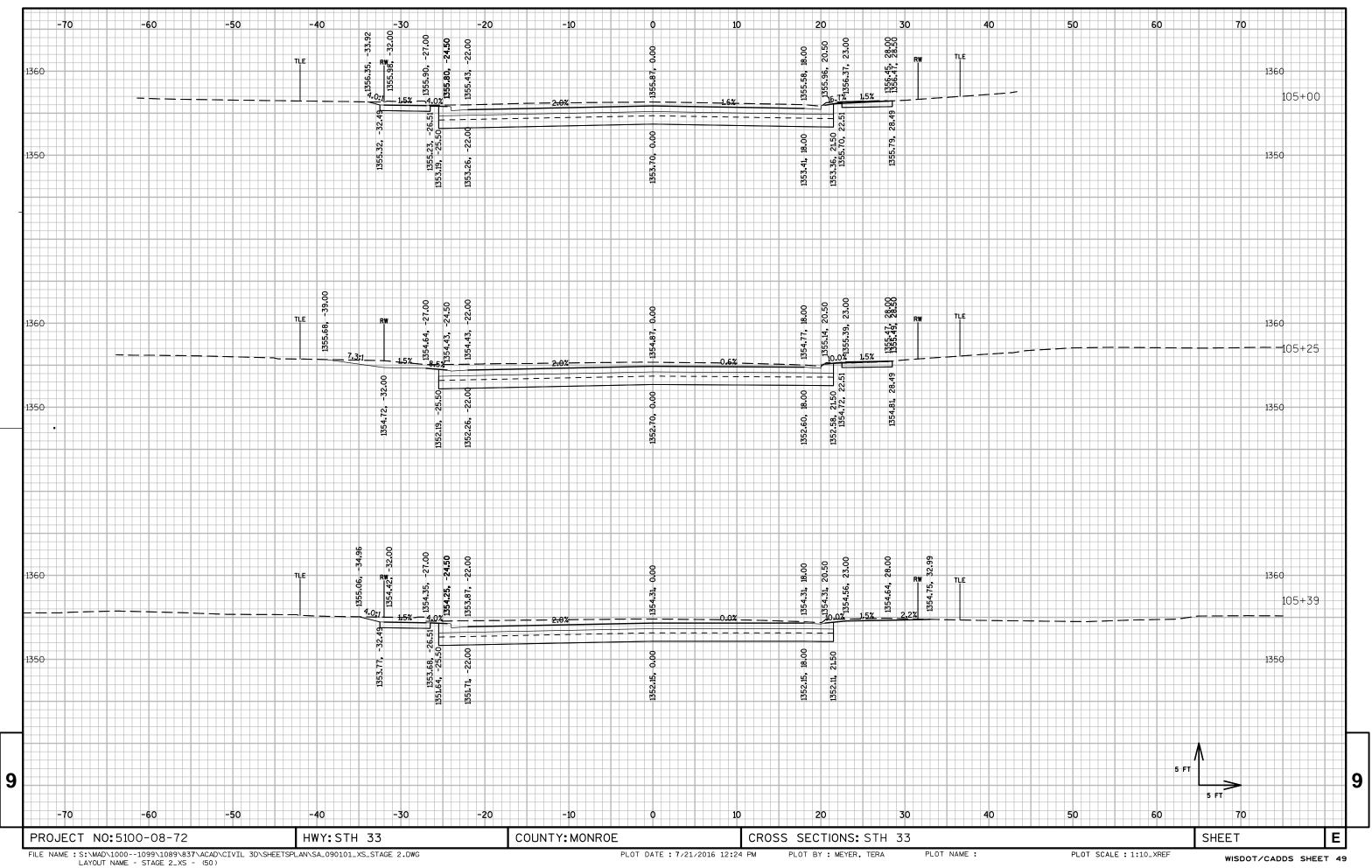


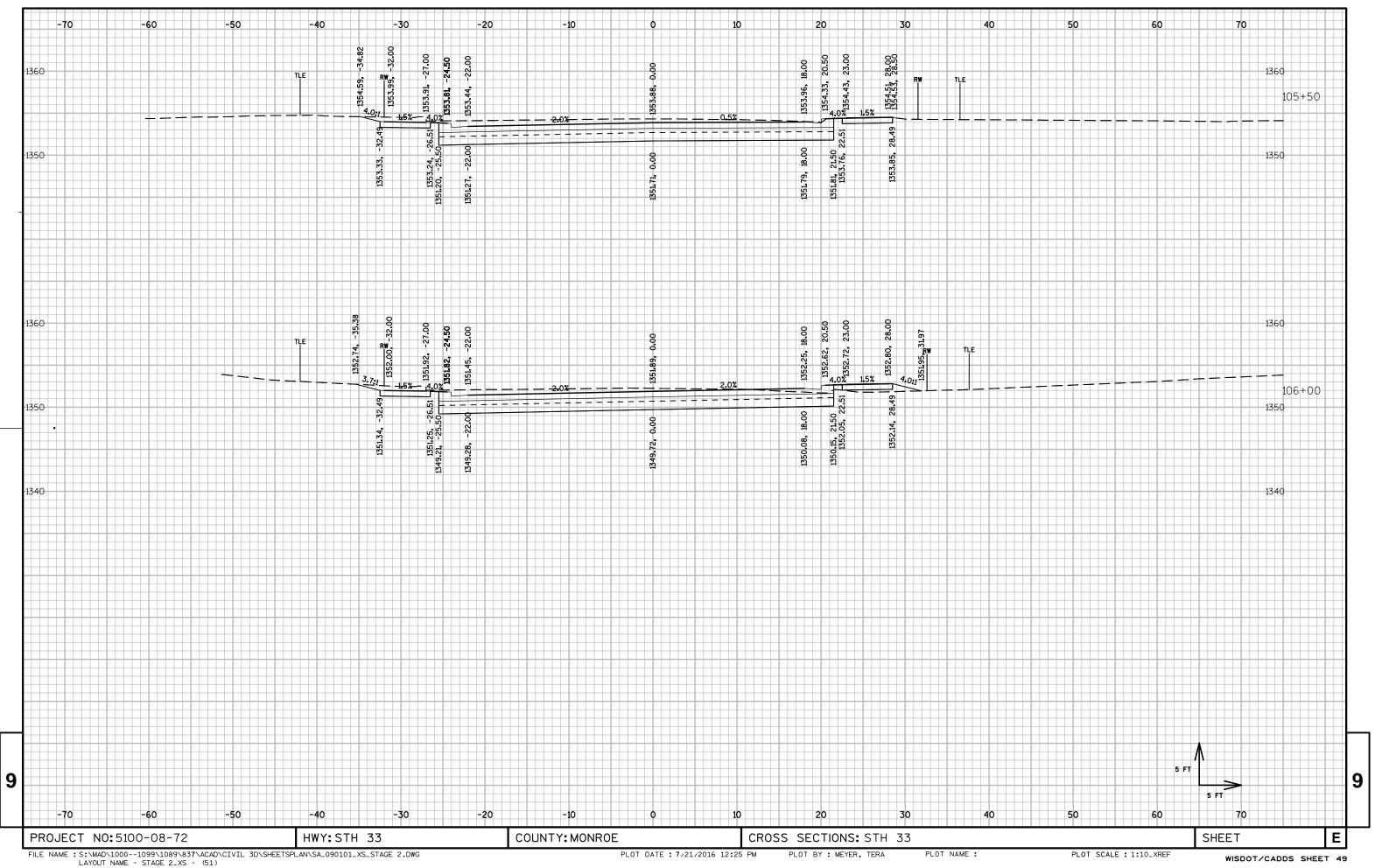


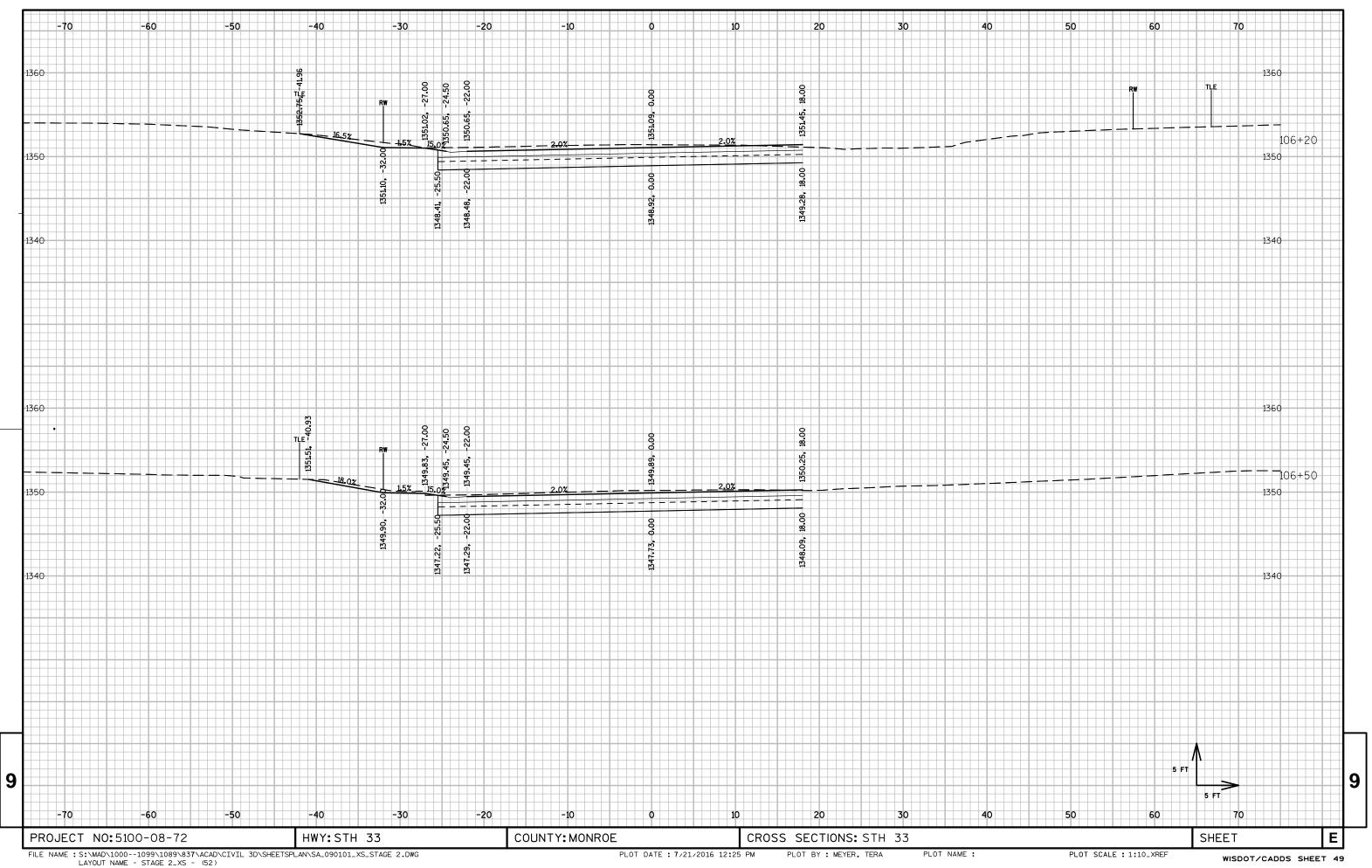


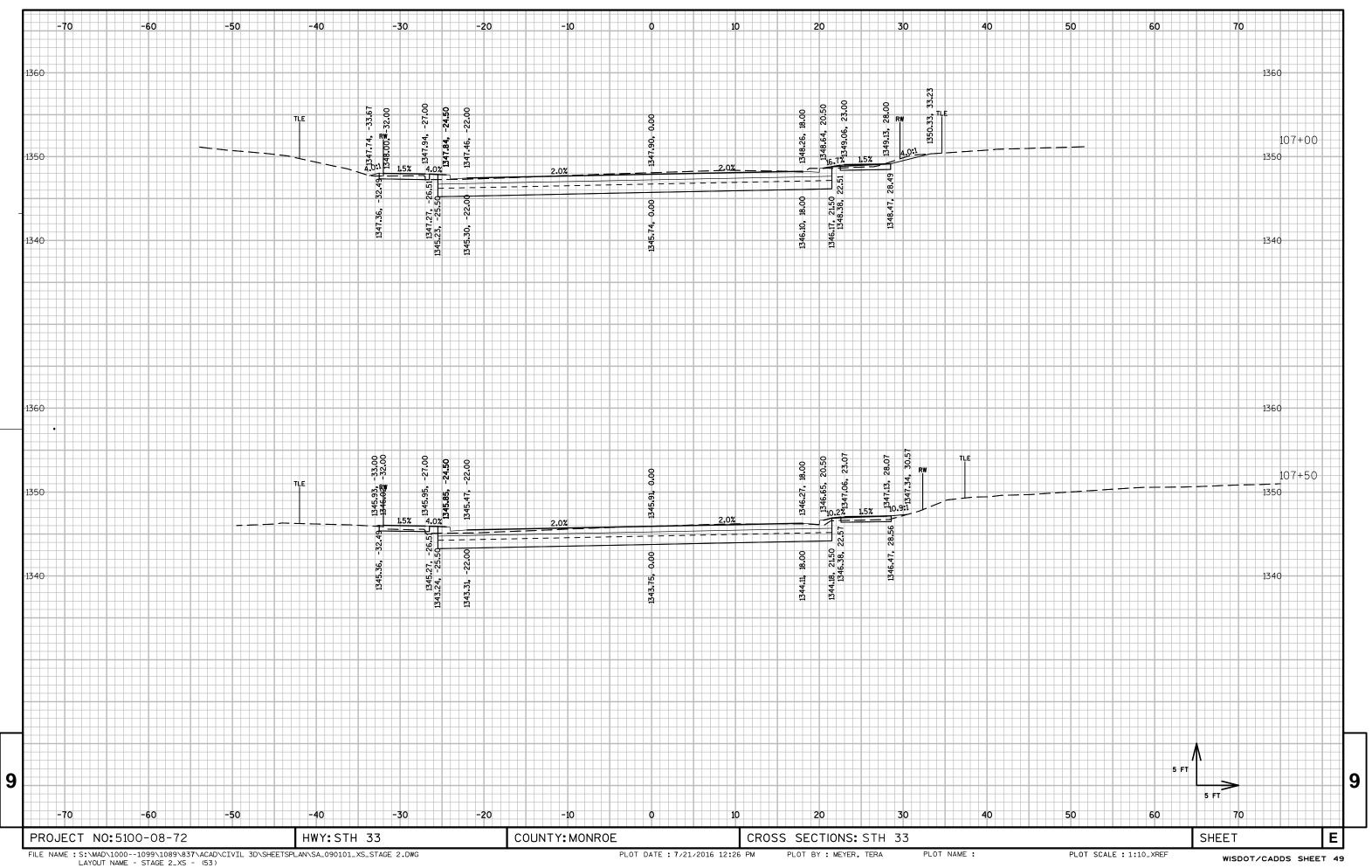


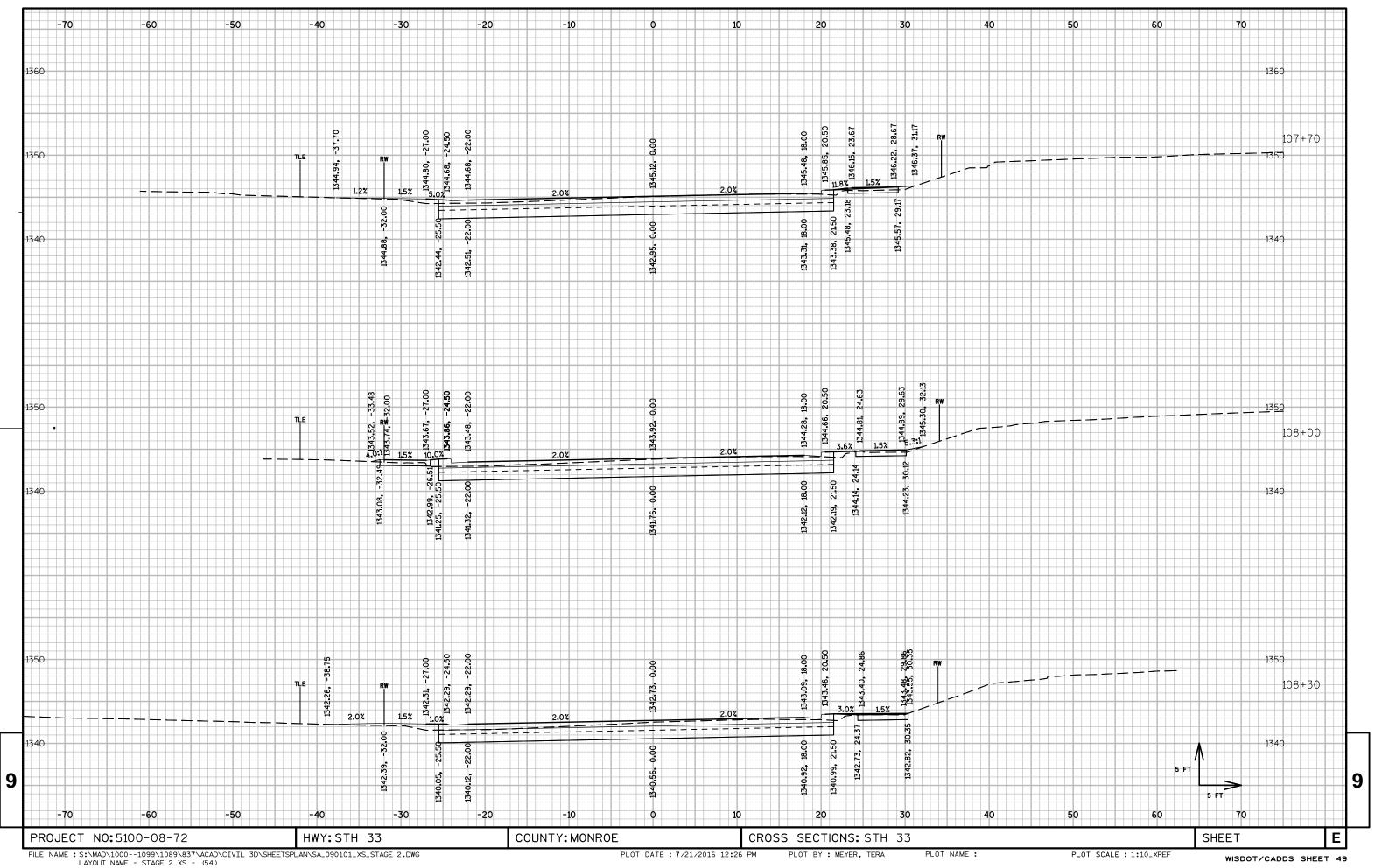


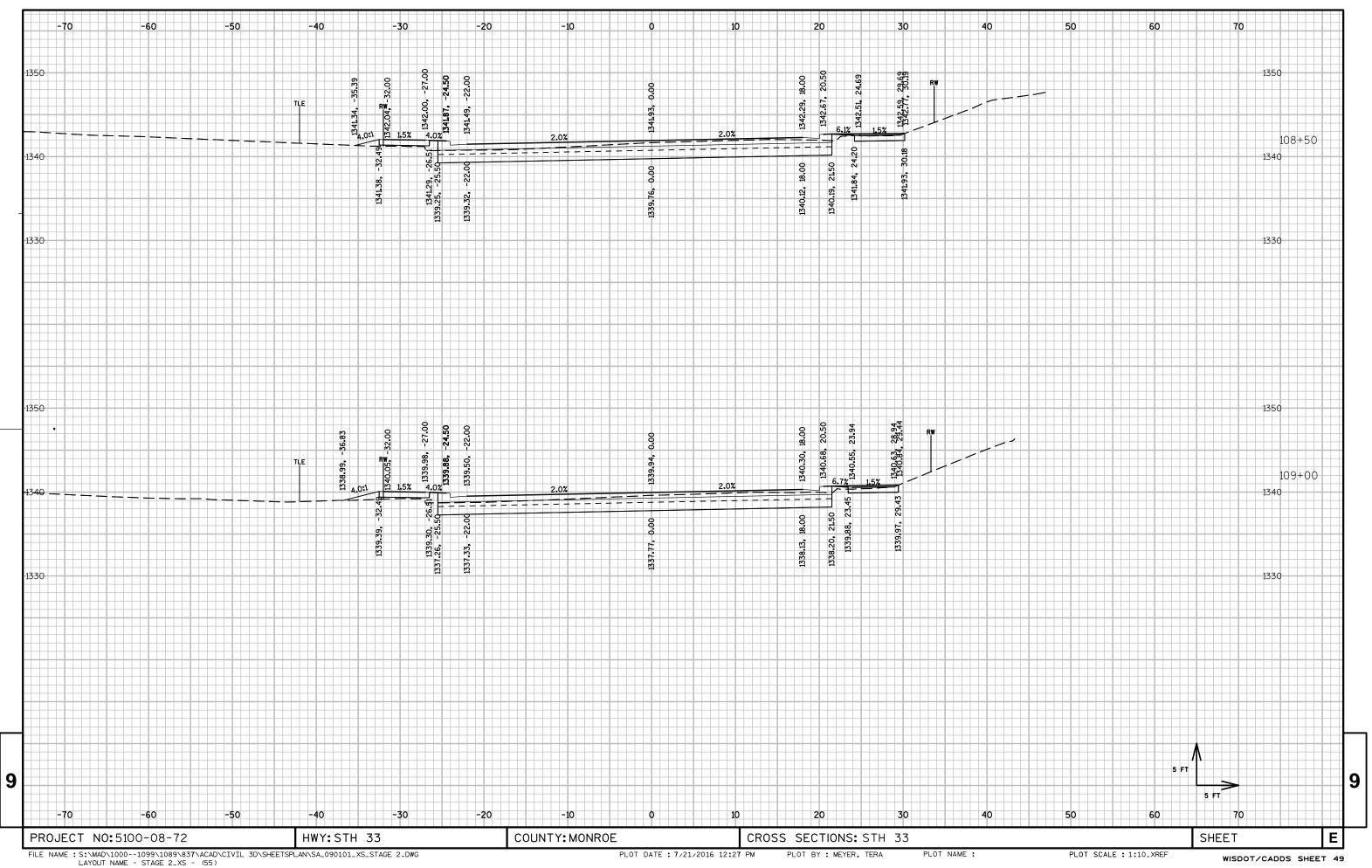


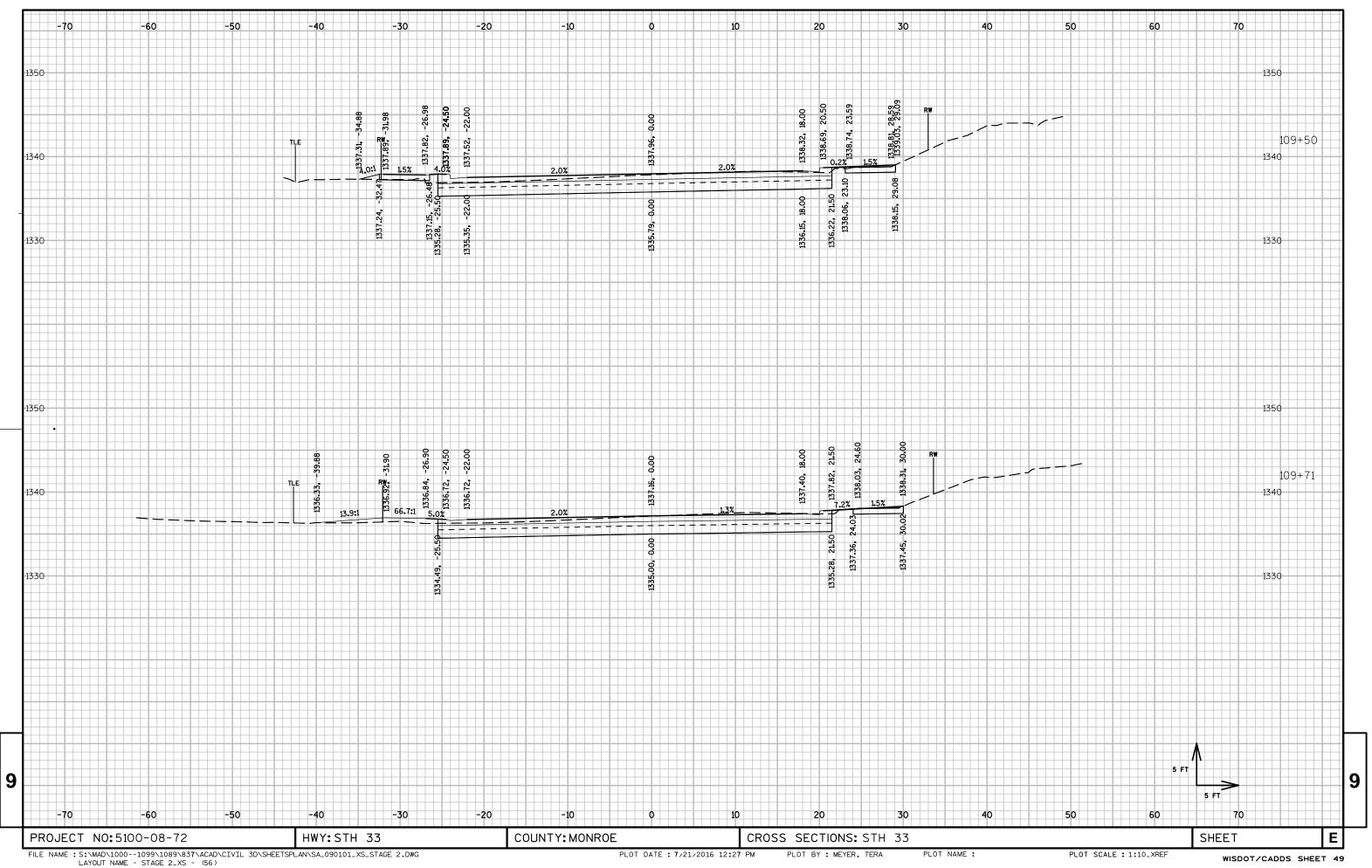


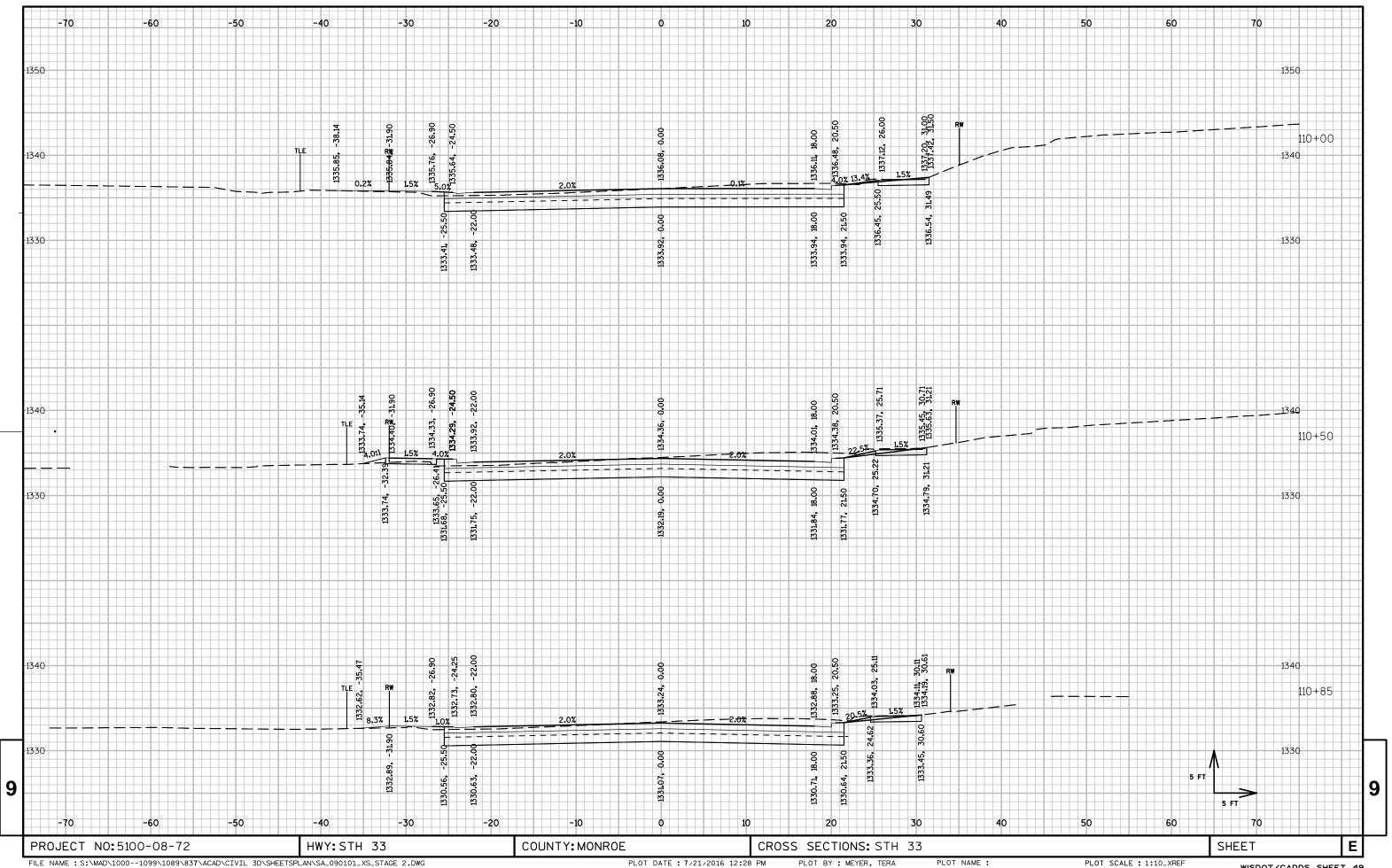


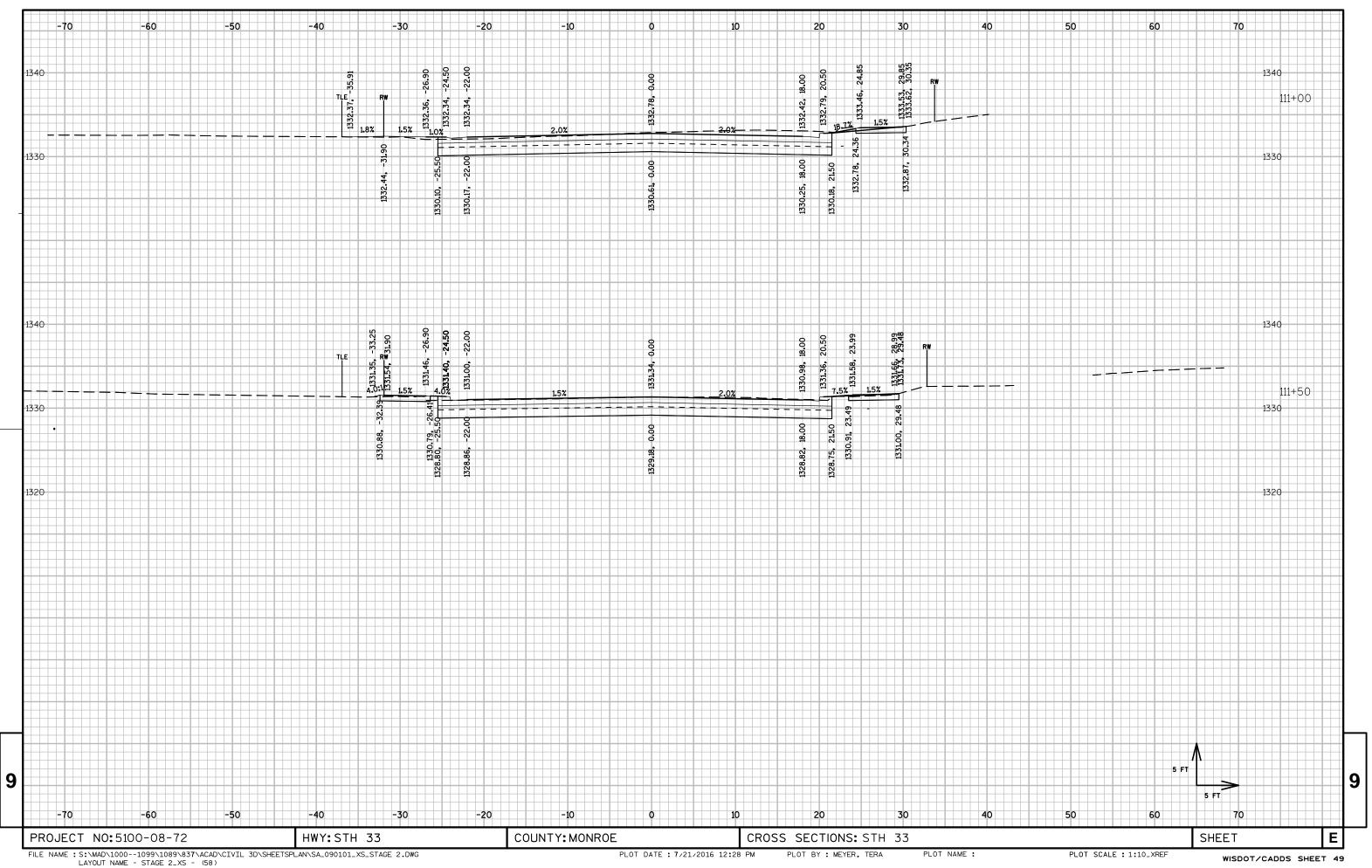


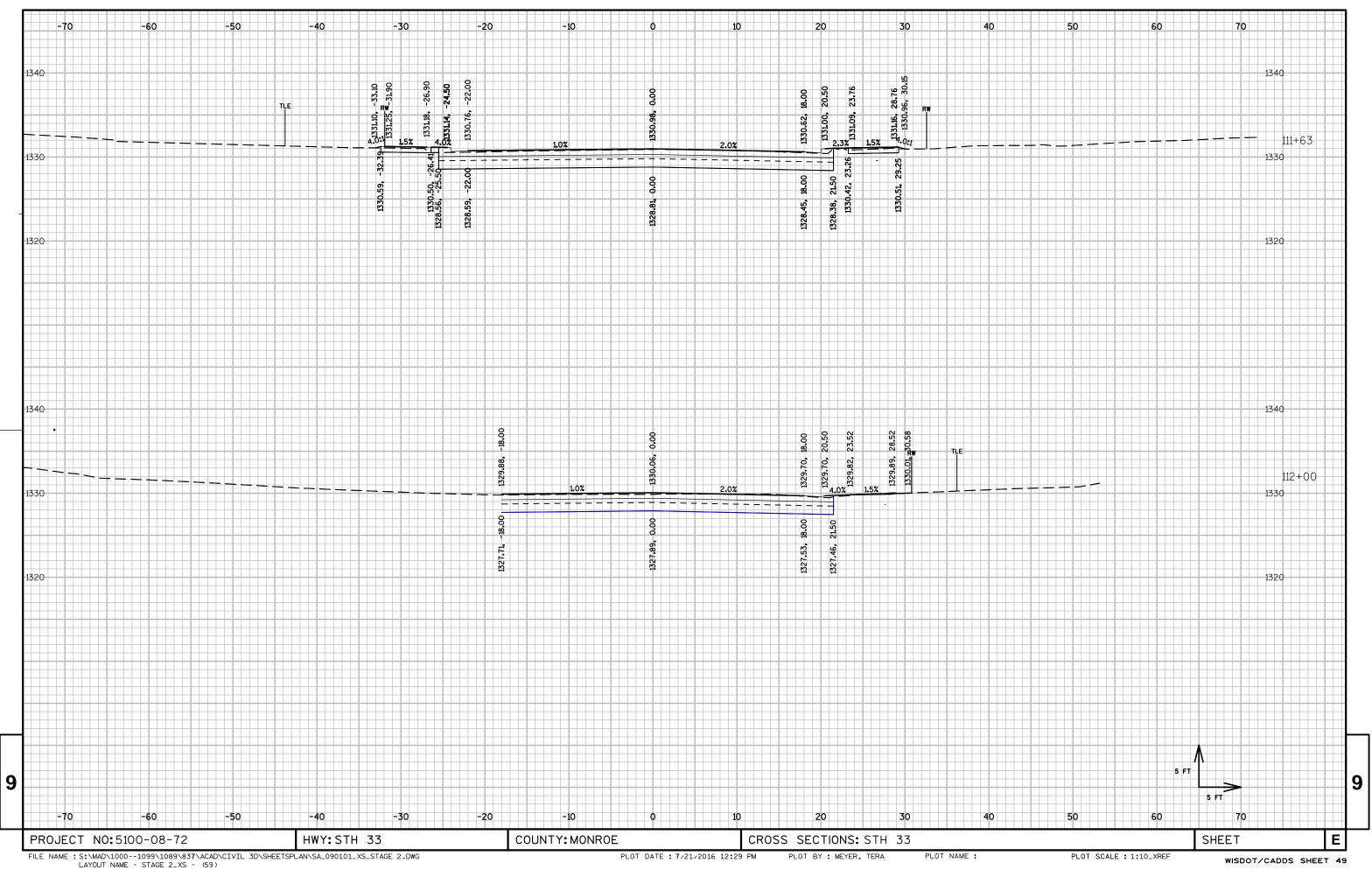


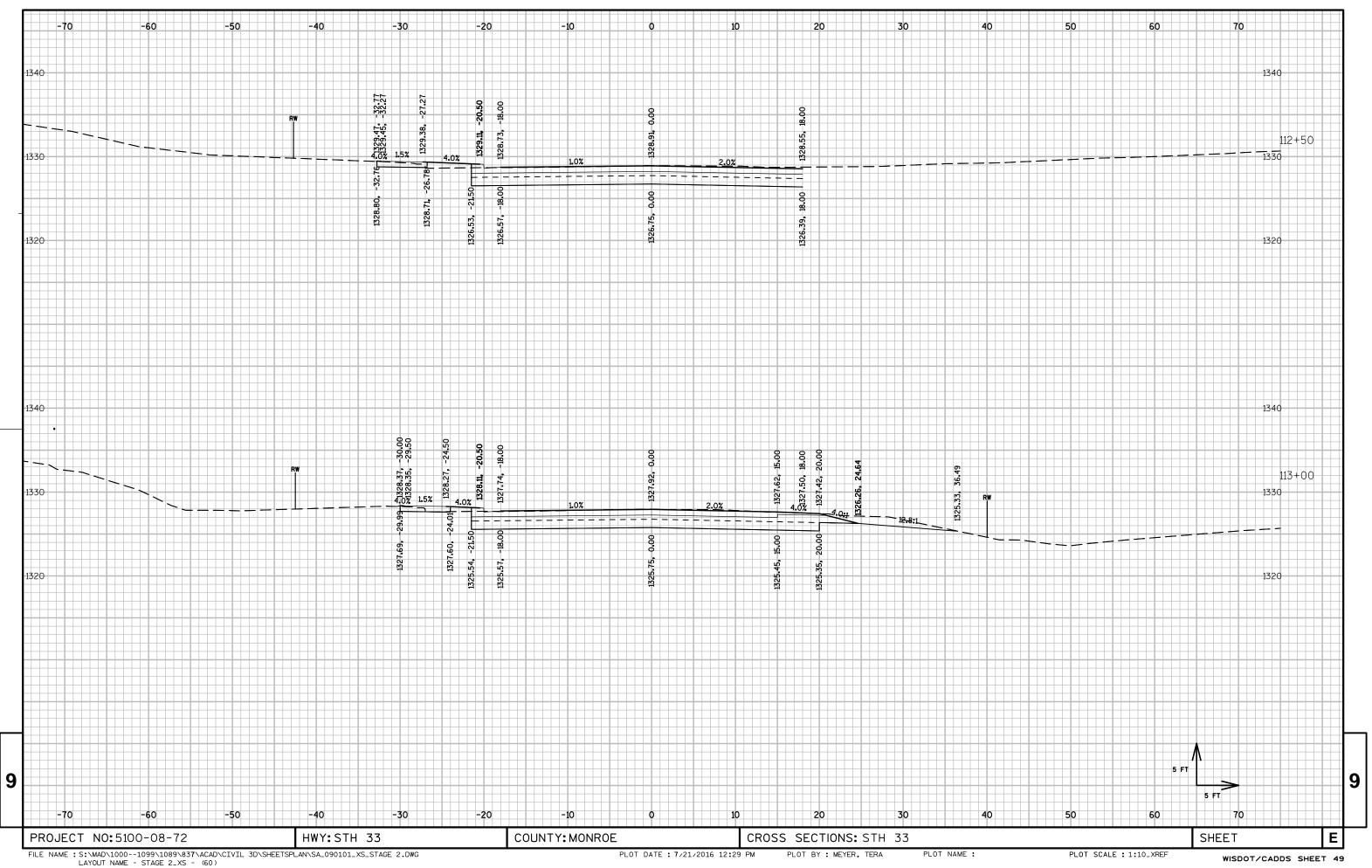


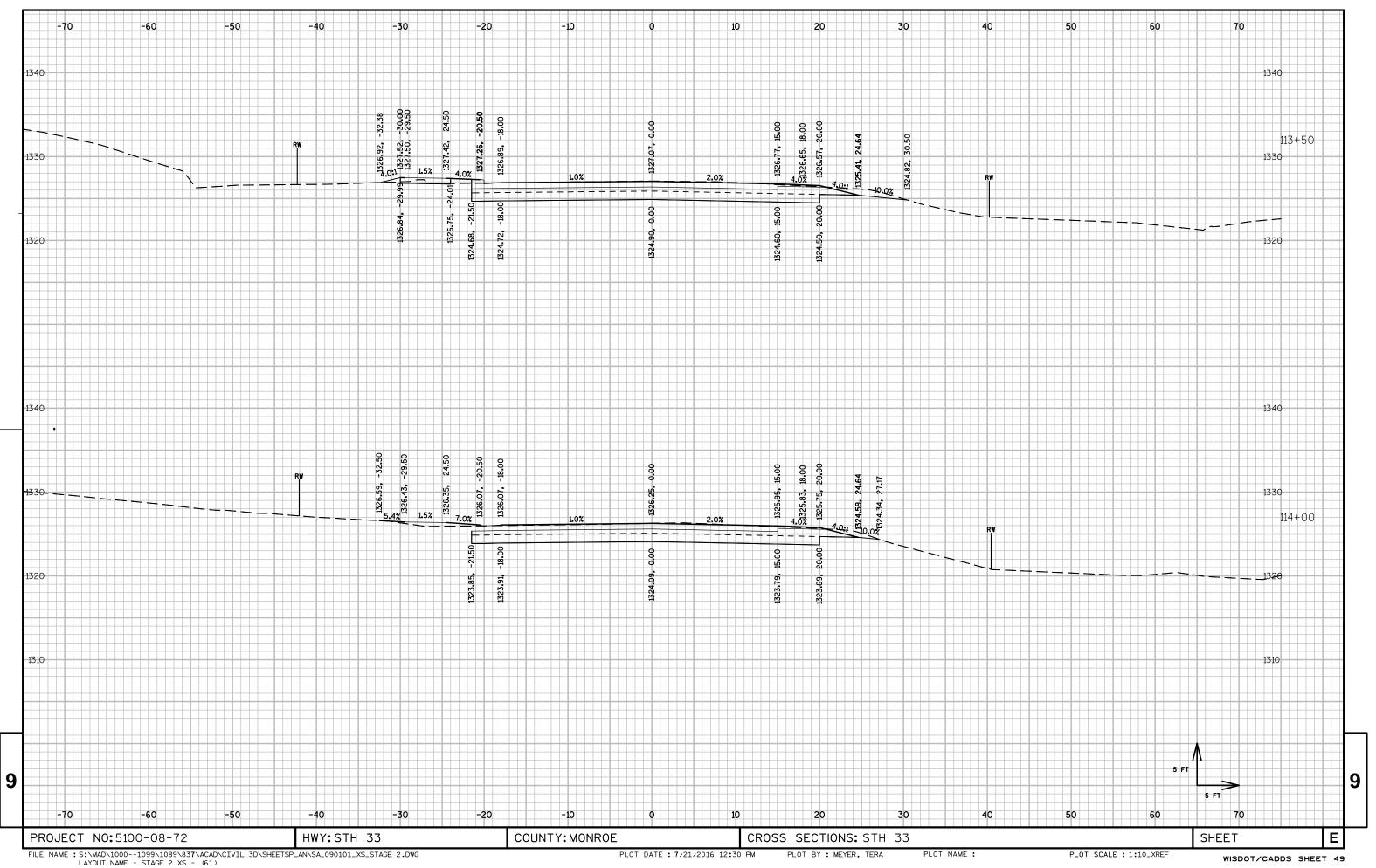


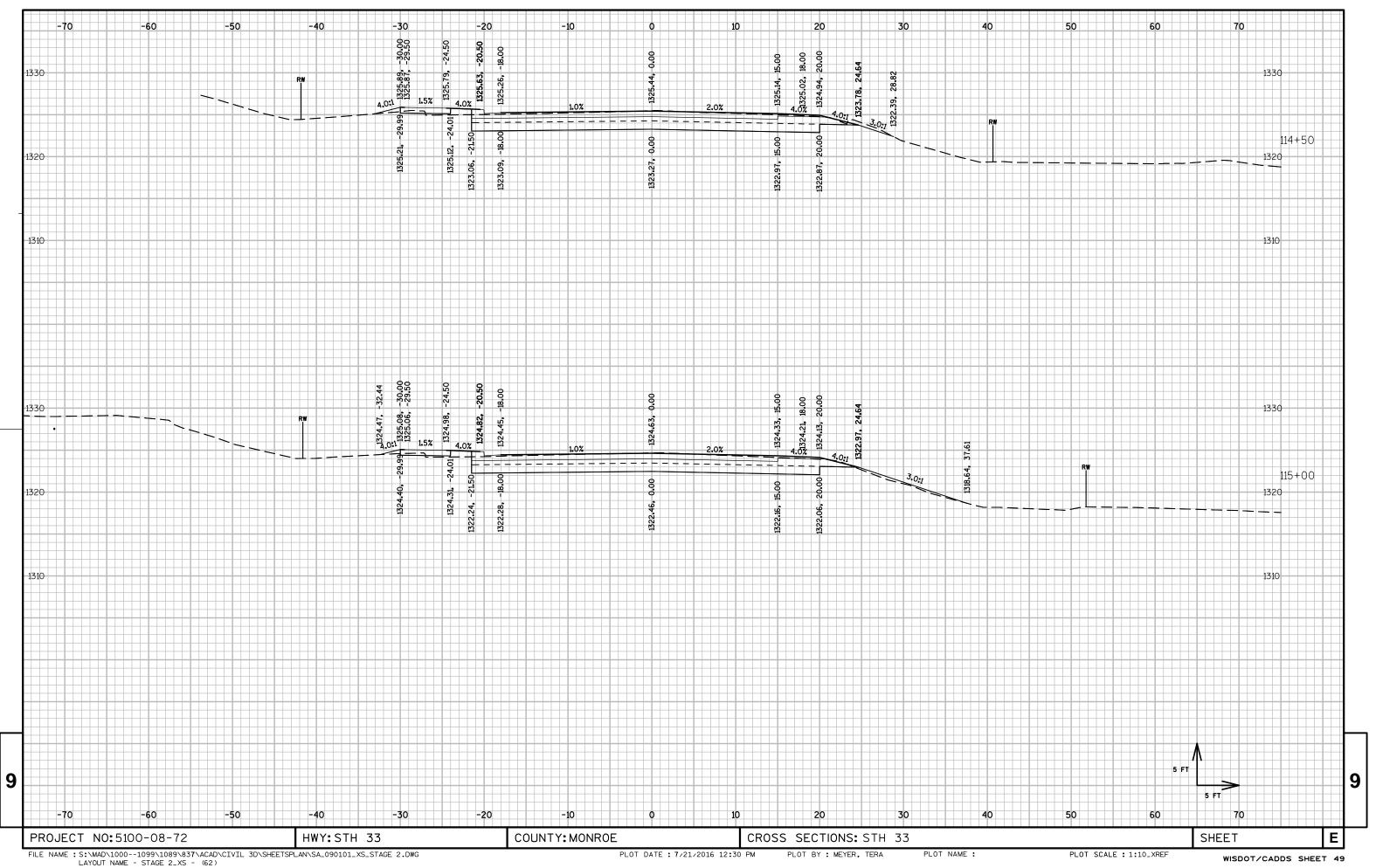


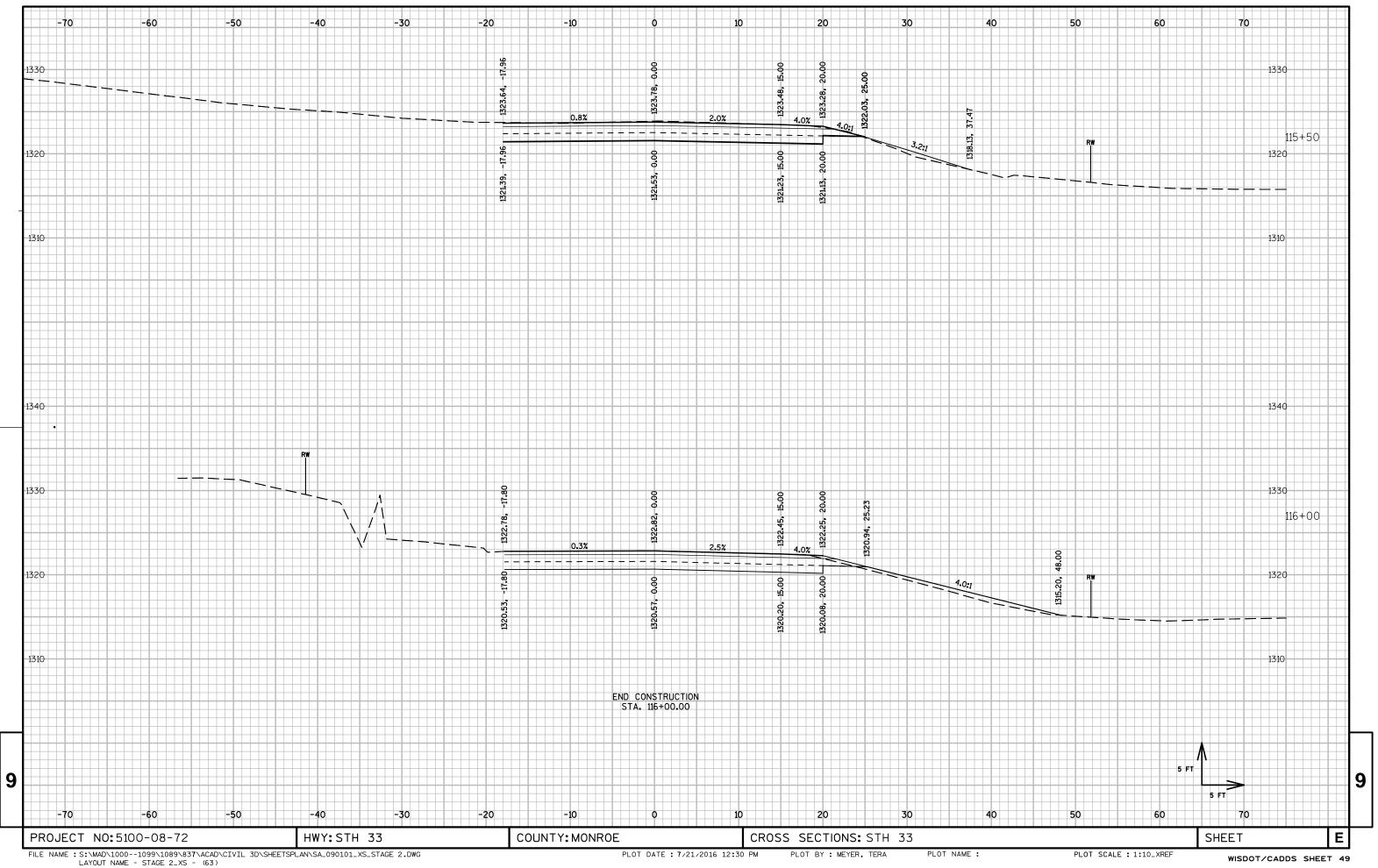


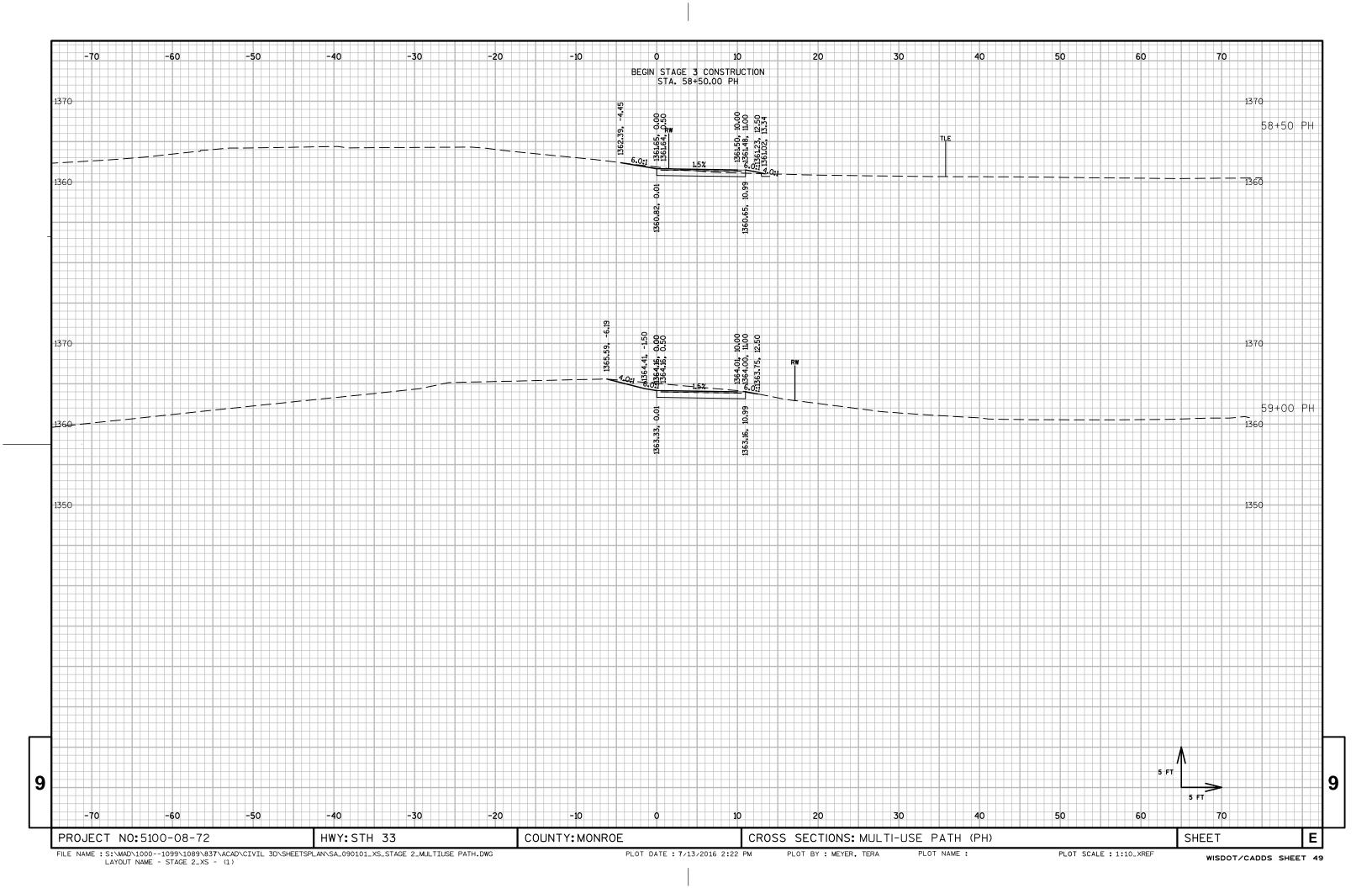


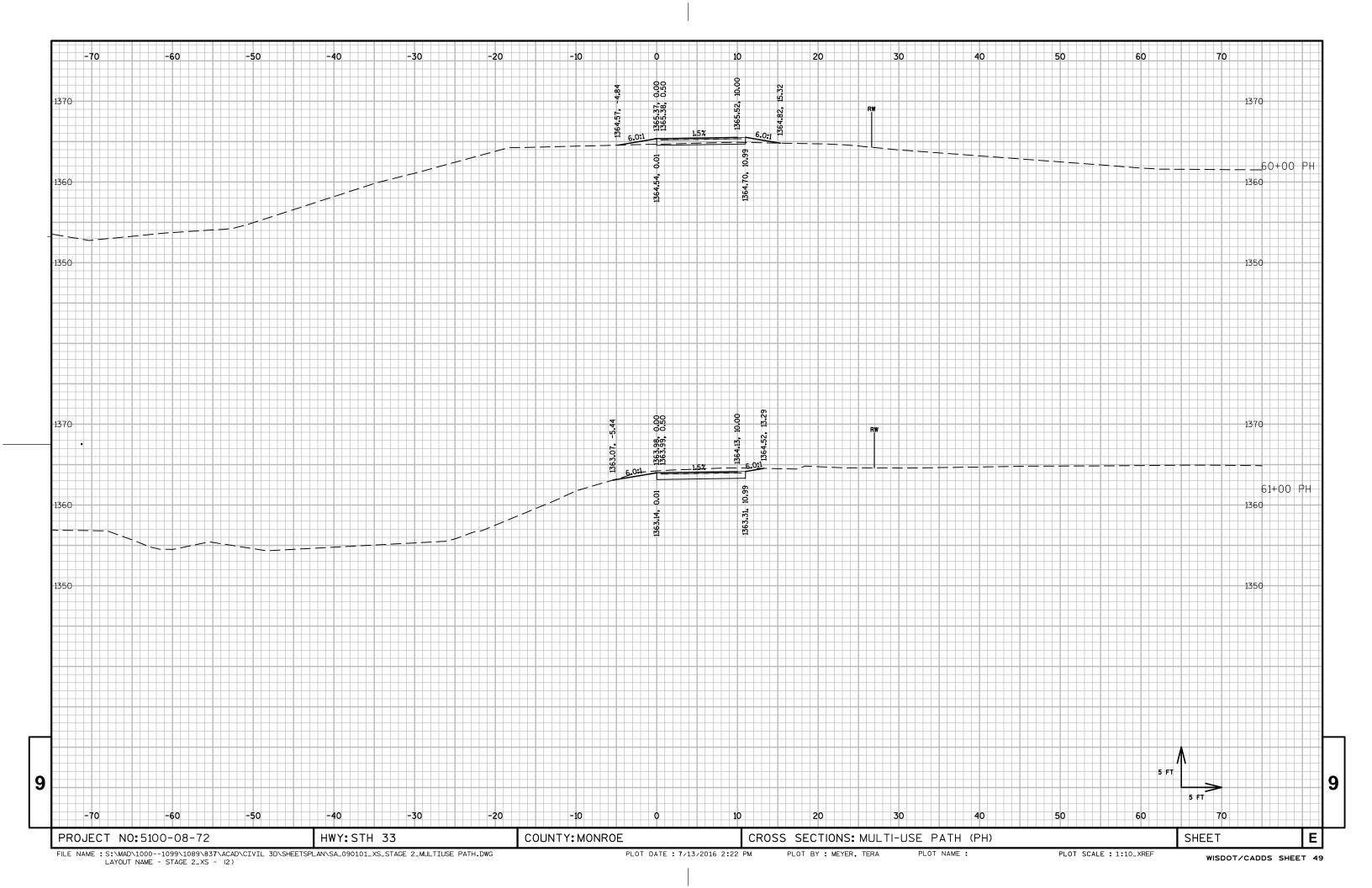


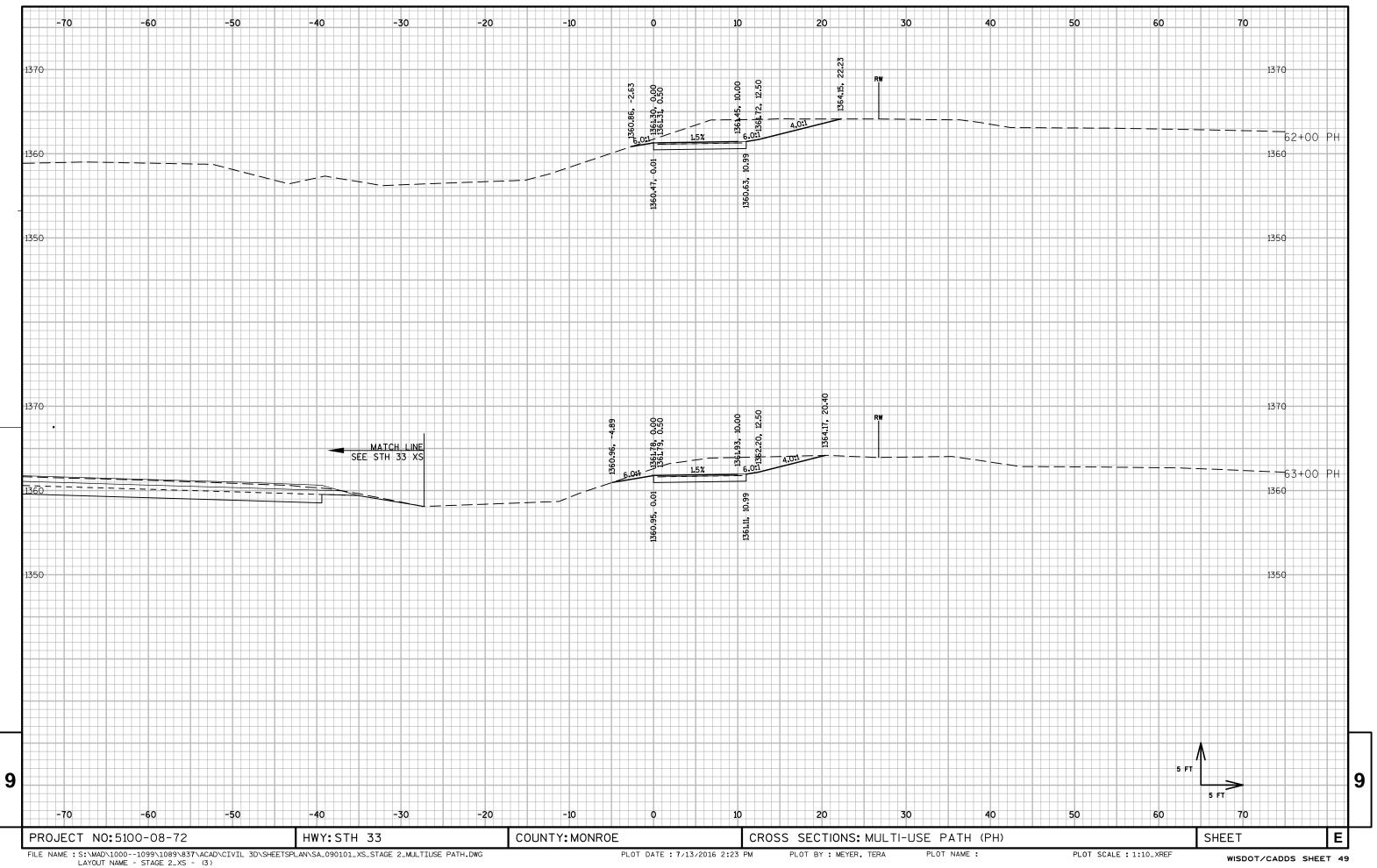


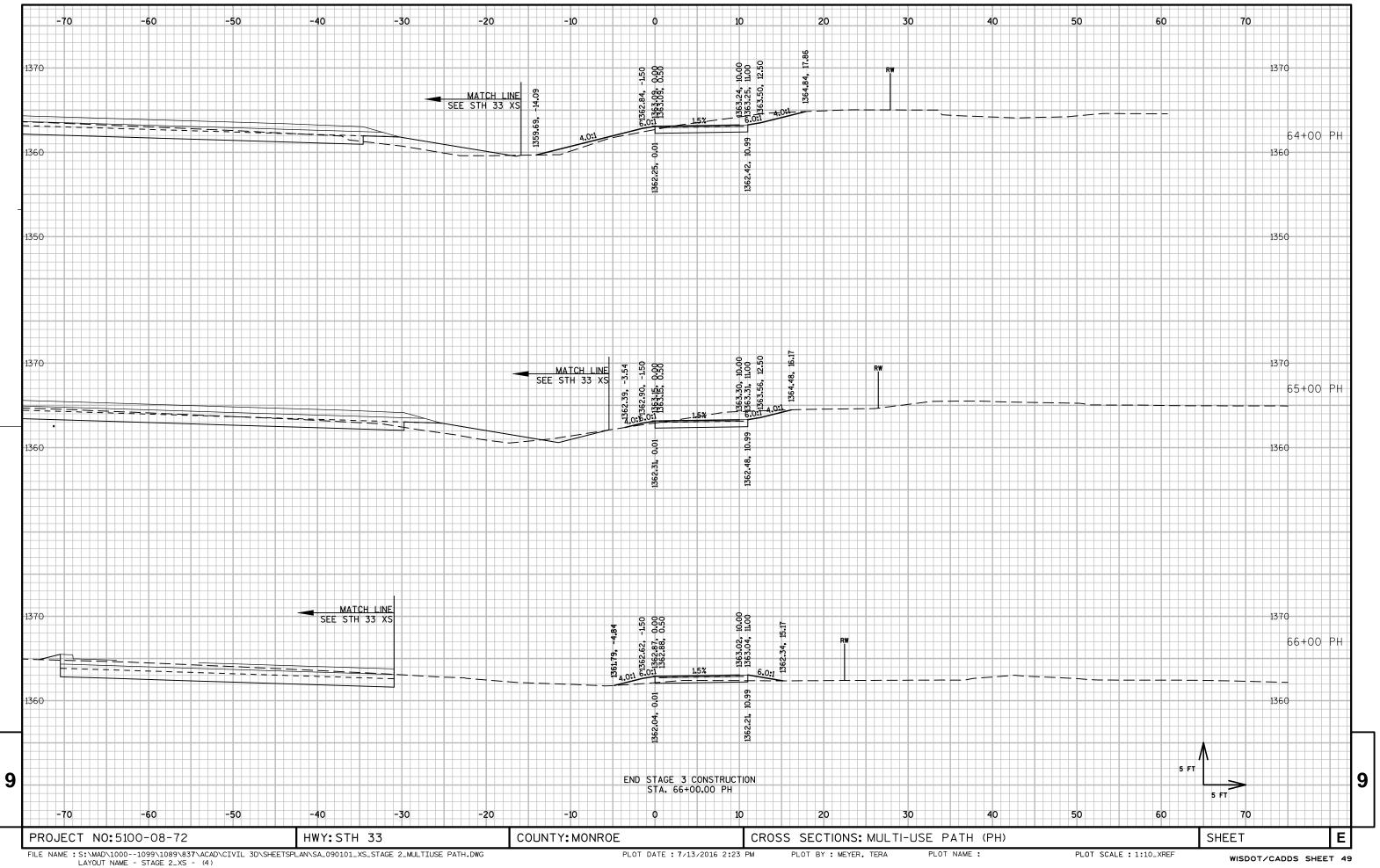














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