

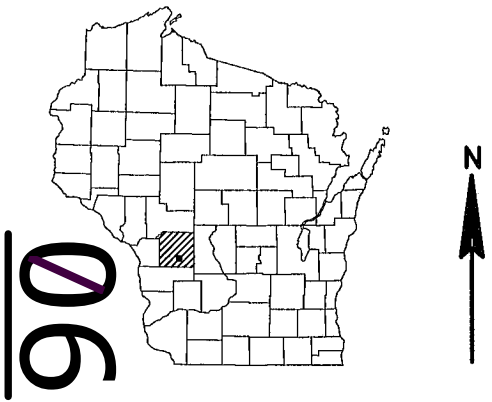
SWL
PROJECT ID: 5119-00-72
WITH: N/A
COUNTY: MONROE

NOV 2016

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile (Includes Erosion Control)
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 30



DESIGN DESIGNATION

A.A.D.T.	2017	=	270
A.A.D.T.	2037	=	300
D.H.V.		=	46
D.D.		=	60/40
T.		=	3.8%
DESIGN SPEED		=	50 MPH
ESALS		=	21,900

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	
EXISTING RIGHT OF WAY		GRADE ELEVATION	
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE		ELECTRIC	
EXISTING CULVERT		FIBER OPTIC	
PROPOSED CULVERT (Box or Pipe)		GAS	
COMBUSTIBLE FLUIDS		SANITARY SEWER	
MARSH AREA		STORM SEWER	
WOODED OR SHRUB AREA		TELEPHONE	
		WATER	
		UTILITY PEDESTAL	
		POWER POLE	
		TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
WILTON - TOMAH
(BR SLEIGHTON CREEK BRIDGE C-41-0136)
CTH M
MONROE COUNTY

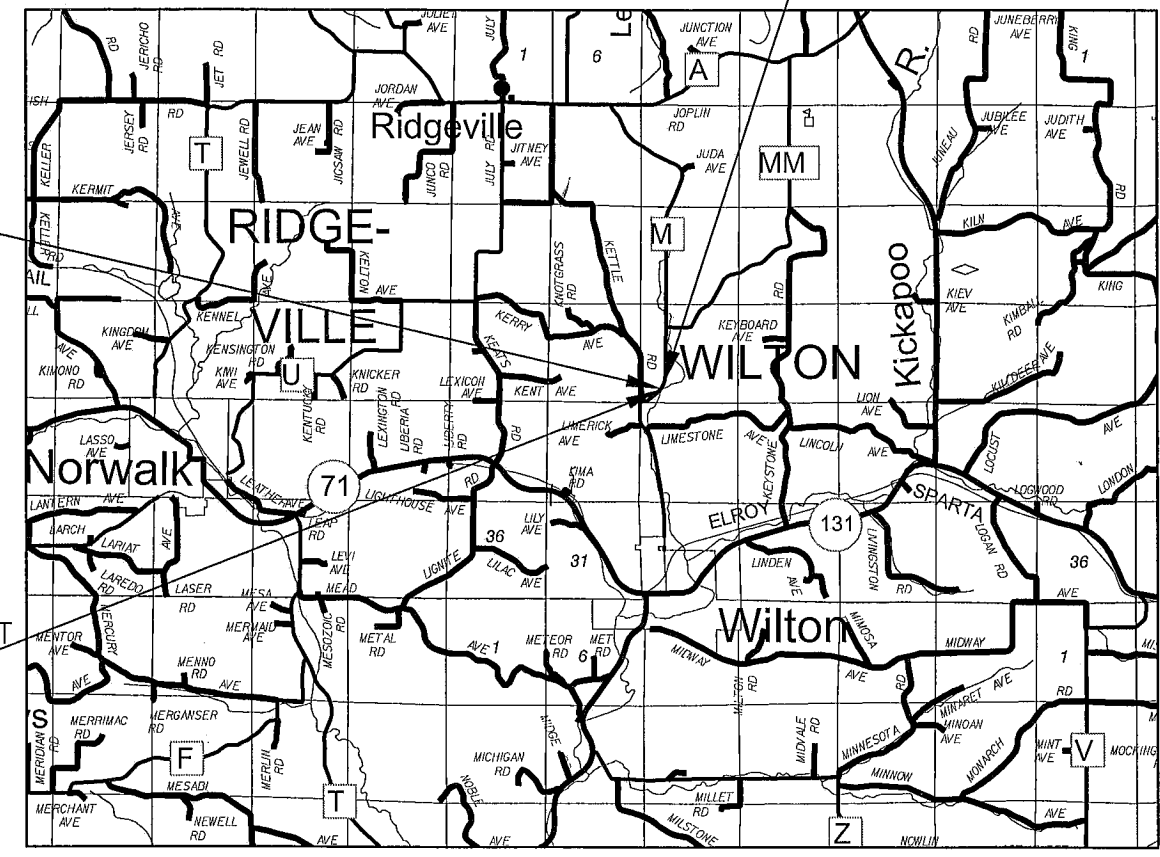
STATE PROJECT NUMBER
5119-00-72

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5119-00-72	WISC 2016341	1

STRUCTURE C-41-0136

BEGIN PROJECT
STA. 8+75
Y = 342,503.18
X = 700,024.08

END PROJECT
STA. 11+50



LAYOUT
SCALE 0 2 MILE
TOTAL NET LENGTH OF CENTERLINE = 0.052 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ACCEPTED FOR
COUNTY OF MONROE

04/08/2016 *Jack Dittman*
(Date) (Highway Commissioner)

ORIGINAL PLANS PREPARED BY
MSA
PROFESSIONAL SERVICES
TRANSPORTATION • MUNICIPAL
DEVELOPMENT • ENVIRONMENTAL
1330 South Boulevard, Baraboo, WI 53003
608-556-2771 1-800-962-4555 Fax: 608-556-2770

WISCONSIN
LEAH J. RHODES
E-41726
BARABOO
WI
PROFESSIONAL ENGINEER
DATE: 4/4/2016 *Leah J. Rhodes*
(Professional Engineer)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PREPARED BY
Surveyor MSA PROFESSIONAL SERVICES, INC.
Designer MSA PROFESSIONAL SERVICES, INC.
Management Consultant KL ENGINEERING, INC.

APPROVED FOR THE DEPARTMENT
DATE: 4/27/16 *Jeff Melville*
(Manager/Consultant Signature)

STANDARD ABBREVIATIONS

AC	ACRE	F/L	FLOW LINE	SALV	SALVAGED
AGG	AGGREGATE	FT	FOOT	SAN	SANITARY SEWER
<	ANGLE	GN	GRID NORTH	SECT	SECTION
ASPH	ASPHALTIC	HR	HANDICAP RAMP	SHLDR	SHOULDER
AC	ASPHALT CEMENT	HT	HEIGHT	SW	SIDEWALK
ADT	AVERAGE DAILY TRAFFIC	CWT	HUNDREDWEIGHT	S	SOUTH
B & B	BALLED AND BURLAPPED	HYD	HYDRANT	SB	SOUTHBOUND
BM	BENCH MARK	IN DIA	INCH DIAMETER	SPECS	SPECIFICATIONS
CB	CATCH BASIN	INL	INLET	SQ	SQUARE
℄ OR C/L	CENTER LINE	ID	INSIDE DIAMETER	SF OR SQ FT	SQUARE FEET
C-C	CENTER TO CENTER	I	INTERSECTION ANGLE	SY	SQUARE YARD
CONC	CONCRETE	IE	INVERT ELEVATION	SSPRC	STORM SEWER
CO	COUNTY	IP	IRON PIPE OR PIN		PIPE REINFORCED CONCRETE
CTH	COUNTY TRUNK HIGHWAY	JCT	JUNCTION	STD	STANDARD
CY	CUBIC YARD	L	LENGTH OF CURVE	SDD	STANDARD DETAIL DRAWINGS
CULV	CULVERT	LF	LINEAR FOOT	STH	STATE TRUNK HIGHWAYS
CP	CULVERT PIPE	LC	LONG CHORD OF CURVE	STA	STATION
CPRC	CULVERT PIPE	LCB	LONG CHORD BEARING	SS	STORM SEWER
	REINFORCED CONCRETE	LS	LUMP SUM	T	TANGENT
C & G	CURB AND GUTTER	MH	MANHOLE	TEL	TELEPHONE
D	DEGREE OF CURVE	N	NORTH	TEMP	TEMPORARY
DHV	DESIGN HOUR VOLUME	Y	NORTH GRID COORDINATE	TLE	TEMPORARY LIMITED EASEMENT
DIA OR ϕ	DIAMETER	OE	OUTLET ELEVATION	T	TON
DIST	DISTRICT	OL	OUT LOT	TC	TOP OF CURB
DWY	DRIVEWAY	OD	OUTSIDE DIAMETER	TN	TOWN
E	EAST	OH	OVERHEAD LINES	TRANS	TRANSITION
X	EAST GRID COORDINATE	PAVT	PAVEMENT	T	TRUCKS (percent of)
EB	EASTBOUND	PLE	PERMANENT LIMITED EASEMENT	TYP	TYPICAL
ELEC	ELECTRIC	PC	POINT OF CURVATURE	UNCL	UNCLASSIFIED
EL OR ELEV	ELEVATION	PI	POINT OF INTERSECTION	USH	UNITED STATES HIGHWAY
EMB	EMBANKMENT	PT	POINT OF TANGENCY	VAR	VARIABLE
EW	ENDWALL	PCC	PORTLAND CEMENT CONCRETE	VERT	VERTICAL
ESALS	EQUIVALENT SINGLE	LB	POUND	VC	VERTICAL CURVE
	AXLE LOADS	PE	PRIVATE ENTRANCE	VOL	VOLUME
EXC	EXCAVATION	R OR RAD	RADIUS	WM	WATER MAIN
EBS	EXCAVATION BELOW	RR	RAILROAD	WV	WATER VALVE
	SUBGRADE	R	RANGE	W	WEST
EXIST	EXISTING	℄ OR R/L	REFERENCE LINE	WB	WESTBOUND
EXP	EXPANSION	REQD	REQUIRED	YD	YARD
F-F	FACE TO FACE	RT	RIGHT		
FERT	FERTILIZER	R/W	RIGHT-OF-WAY		
FE	FIELD ENTRANCE	RD	ROAD		

DESIGN CONTACT

MSA PROFESSIONAL SERVICES, INC.
ATTN: LEAH RHODES, PE
1230 SOUTH BOULEVARD
BARABOO, WI 53913
608-355-8945
LRHODES@MSA-PS.COM

COUNTY CONTACT

MONROE COUNTY
ATTN: JACK DITTMAR, COMMISSIONER
803 WASHINGTON STREET
SPARTA, WI 54656
608-269-8740
JACK.DITTMAR@CO.MONROE.WI.US

DNR LIAISON

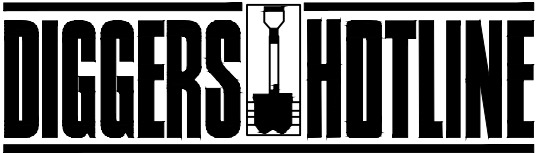
WISCONSIN DEPARTMENT OF
NATURAL RESOURCES
ATTN: KAREN KALVELAGE
ENVIRONMENTAL ANALYSIS AND
REVIEW SPECIALIST
3550 MORMON COULEE ROAD
LA CROSSE, WI 54601
608-785-9115
KAREN.KALVELAGE@WISCONSIN.GOV

UTILITIES

COMMUNICATION:
CENTURYLINK
ATTN: BRET CLARK
311 SOUTH COURT STREET
SPARTA, WI 54656
608-269-0819
BRET.CLARK@CENTURYLINK.COM

ELECTRIC:
OAKDALE ELECTRIC COOPERATIVE
ATTN: TRAVIS CHAMPLIN
489 N OAKWOOD STREET
P.O. BOX 128
OAKDALE, WI 54649
608-372-8848
TRAVISC@OAKDALEREC.COM

* NOT A MEMBER OF
DIGGERS HOTLINE



Dial 811 or (800) 242-8511
www.DiggersHotline.com

GENERAL NOTES

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE FERTILIZED, SEEDED AND MULCHED AS DIRECTED BY THE ENGINEER. OVERSOW PERMANENT SEEDING AREAS WITH TEMPORARY SEED AT 3 LBS. PER 1000 SQUARE FEET.

THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO USGS NAVD 88 (1996 ADJUSTED). BENCHMARKS WERE LOCATED IN THE FIELD USING GPS TECHNOLOGY.

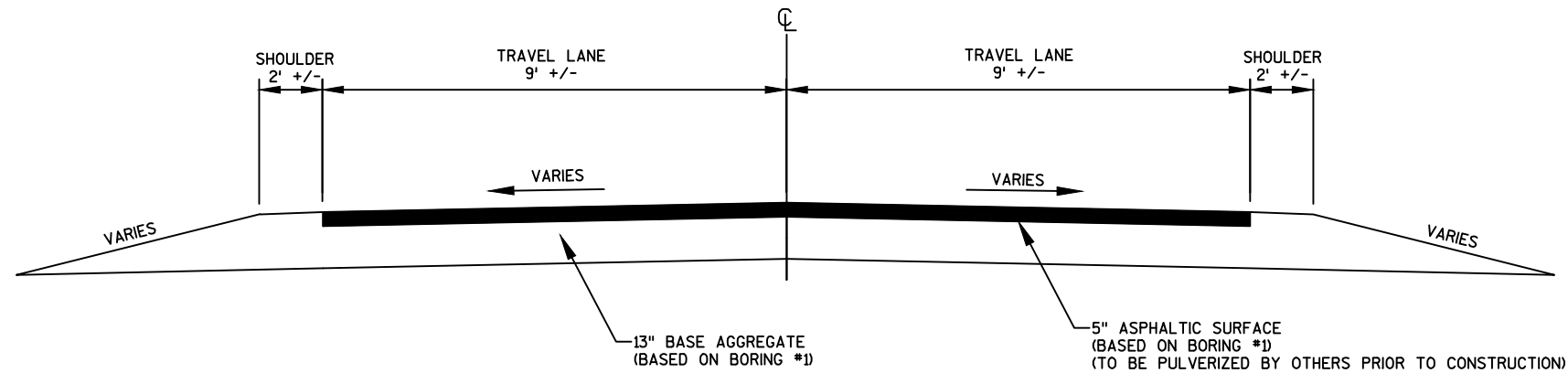
SILT FENCE TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER AND IN PLACE PRIOR TO CONSTRUCTION OR BRIDGE REMOVAL.

WETLANDS ARE PRESENT OUTSIDE THE EXISTING TOE OF SLOPE. AREAS OUTSIDE THE SLOPE INTERCEPTS SHALL NOT BE DISTURBED.

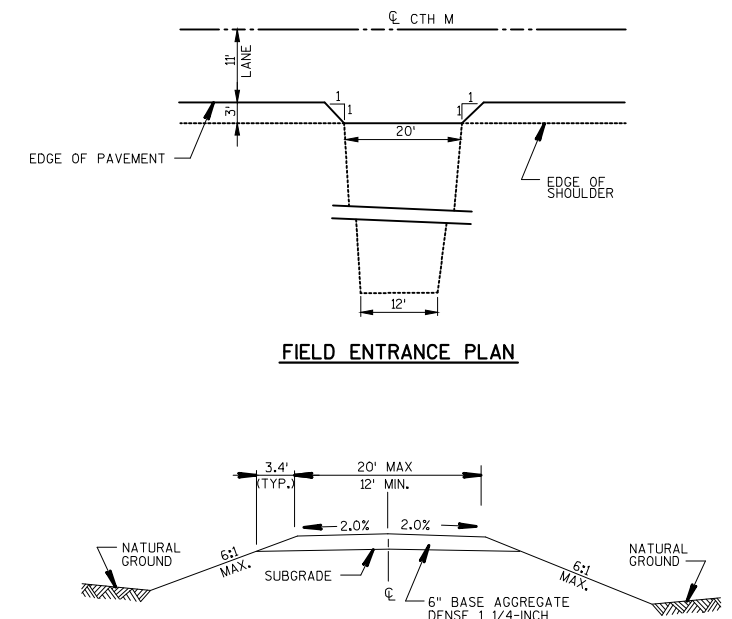
RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIP-TURF	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPE-TURF			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT .70 - .95												
CONCRETE .80 - .95												
BRICK .70 - .80												
DRIVES, WALKS .75 - .85												
ROOFS .75 - .95												
GRAVEL ROADS, SHOULDERS .40 - .60												

TOTAL PROJECT AREA = 0.80 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.58 ACRES

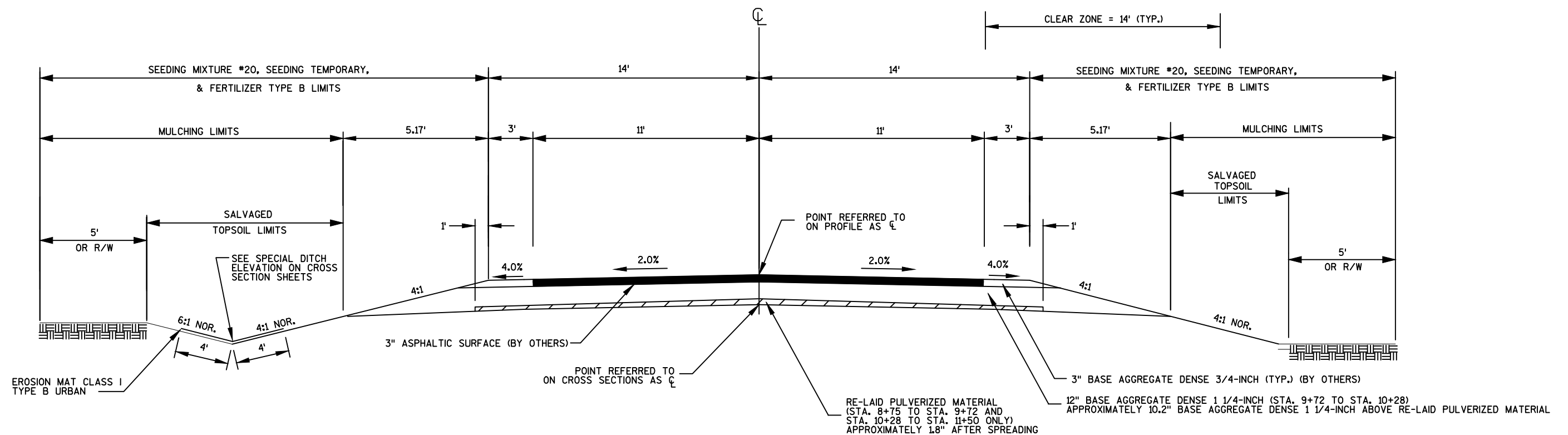


EXISTING TYPICAL SECTION
CTH M



FIELD ENTRANCE TYPICAL SECTION

FIELD ENTRANCE DETAILS



FINISHED TYPICAL SECTION
CTH M

PI = 100+00.00
Y = 342,544.74
X = 700,153.72

PI = 100+14.15
Y = 342,551.03
X = 700,141.05
 $\Delta = 23^\circ 14' 00''$ R
D = 286'28'44"
T = 4.11'
L = 8.11'
R = 20.00'
PC = 100+10.04
PT = 100+18.15

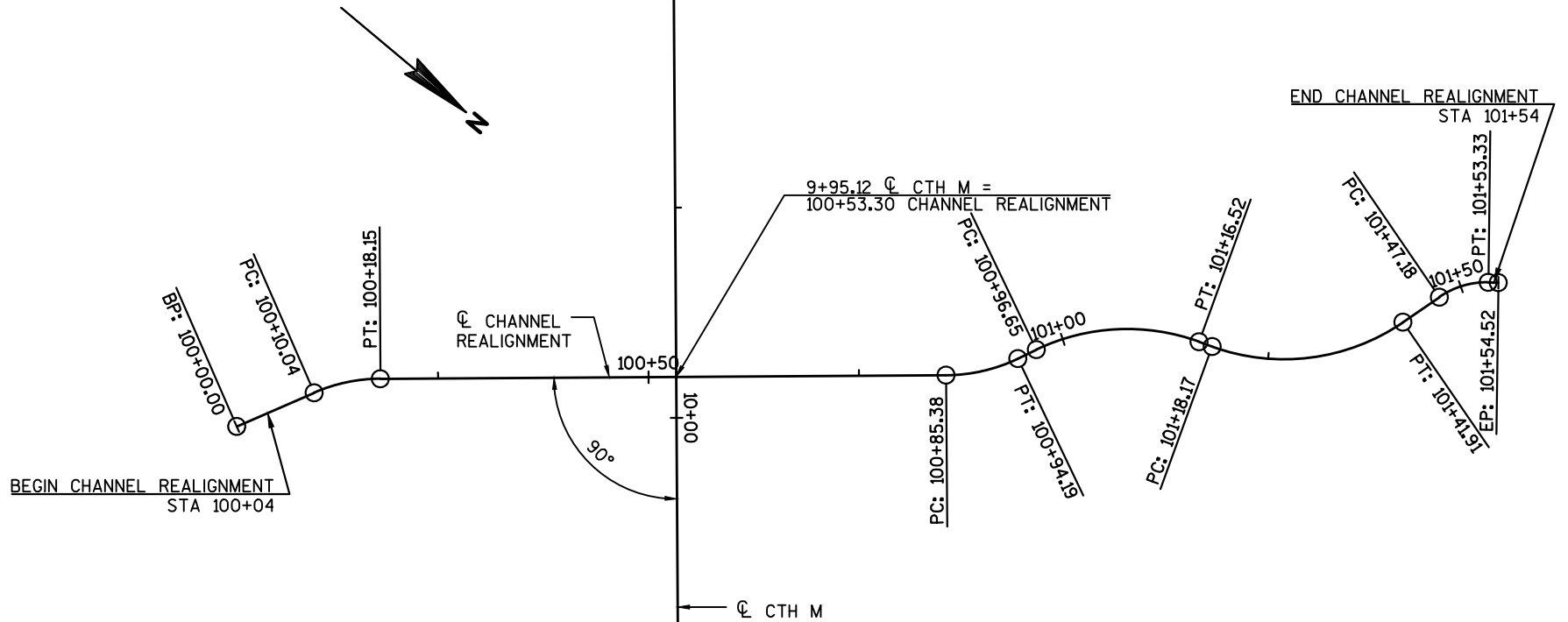
PI = 100+89.86
Y = 342,608.81
X = 700,091.96
 $\Delta = 25^\circ 13' 24''$ L
D = 286'28'44"
T = 4.47'
L = 8.80'
R = 20.00'
PC = 100+85.38
PT = 100+94.19

PI = 101+07.14
Y = 342,616.02
X = 700,076.09
 $\Delta = 45^\circ 31' 52''$ R
D = 229'10'59"
T = 10.49'
L = 19.87'
R = 25.00'
PC = 100+96.65
PT = 101+16.52

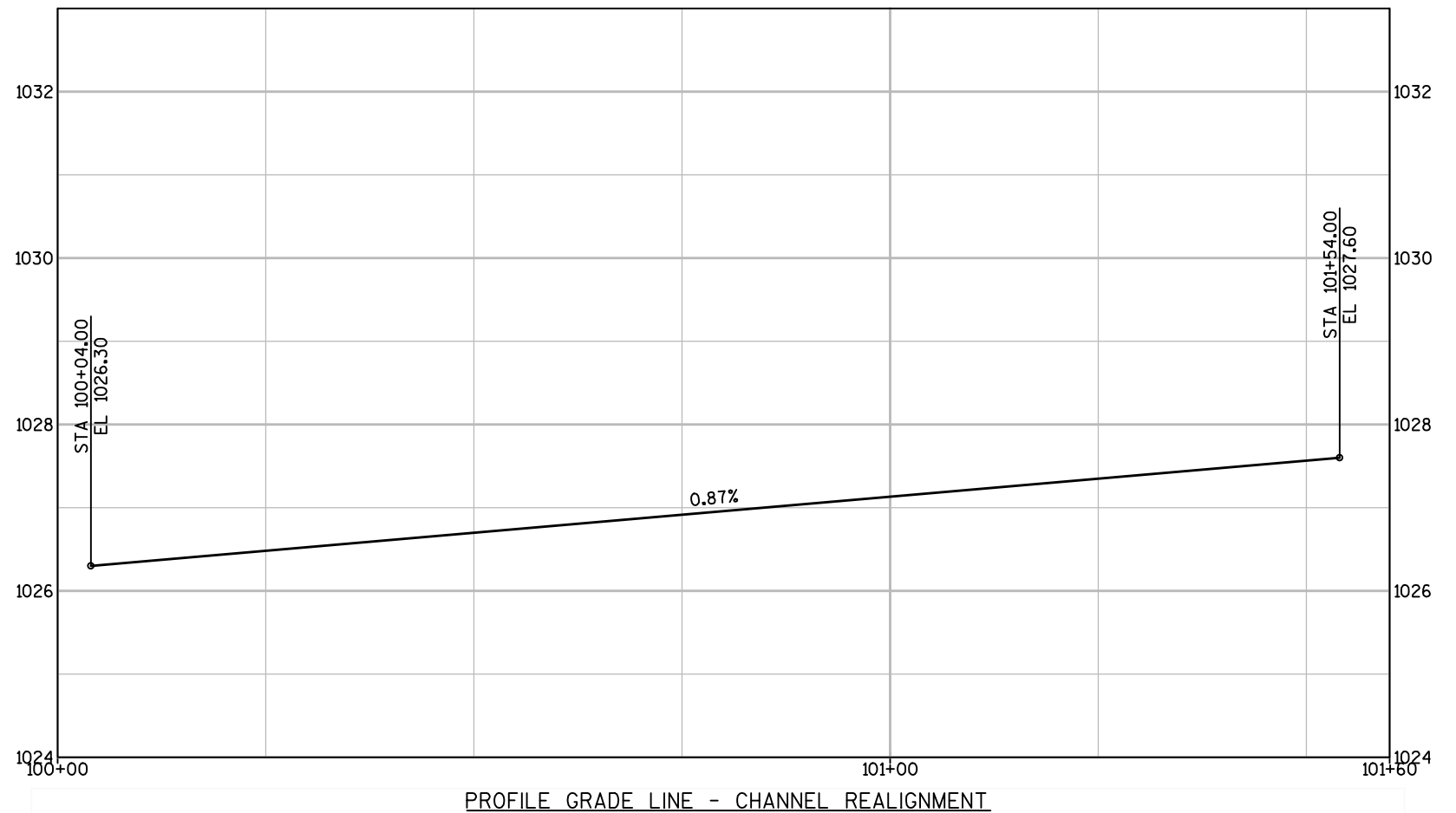
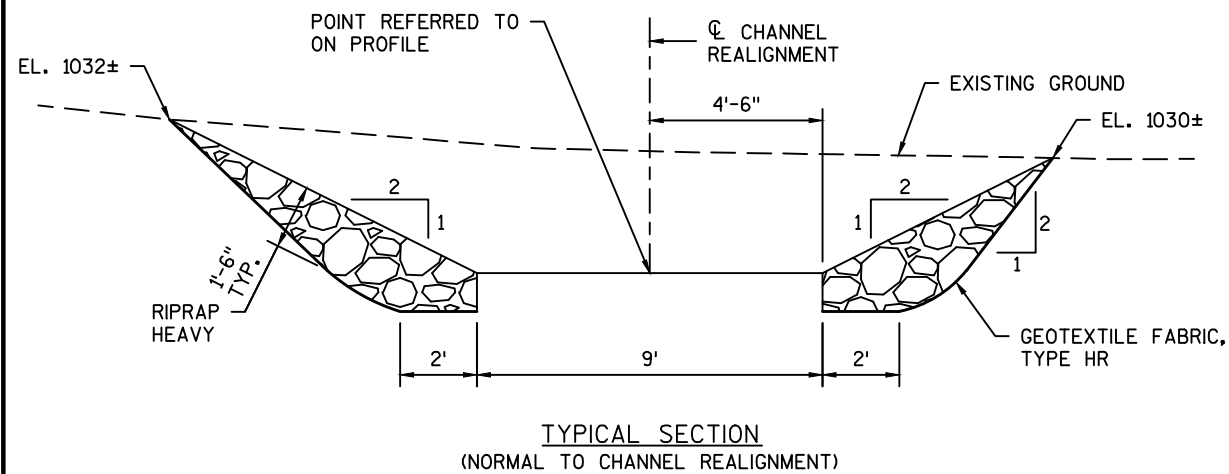
PI = 101+31.02
Y = 342,639.50
X = 700,067.52
 $\Delta = 54^\circ 25' 17''$ L
D = 229'10'59"
T = 12.85'
L = 23.75'
R = 25.00'
PC = 101+18.17
PT = 101+41.91

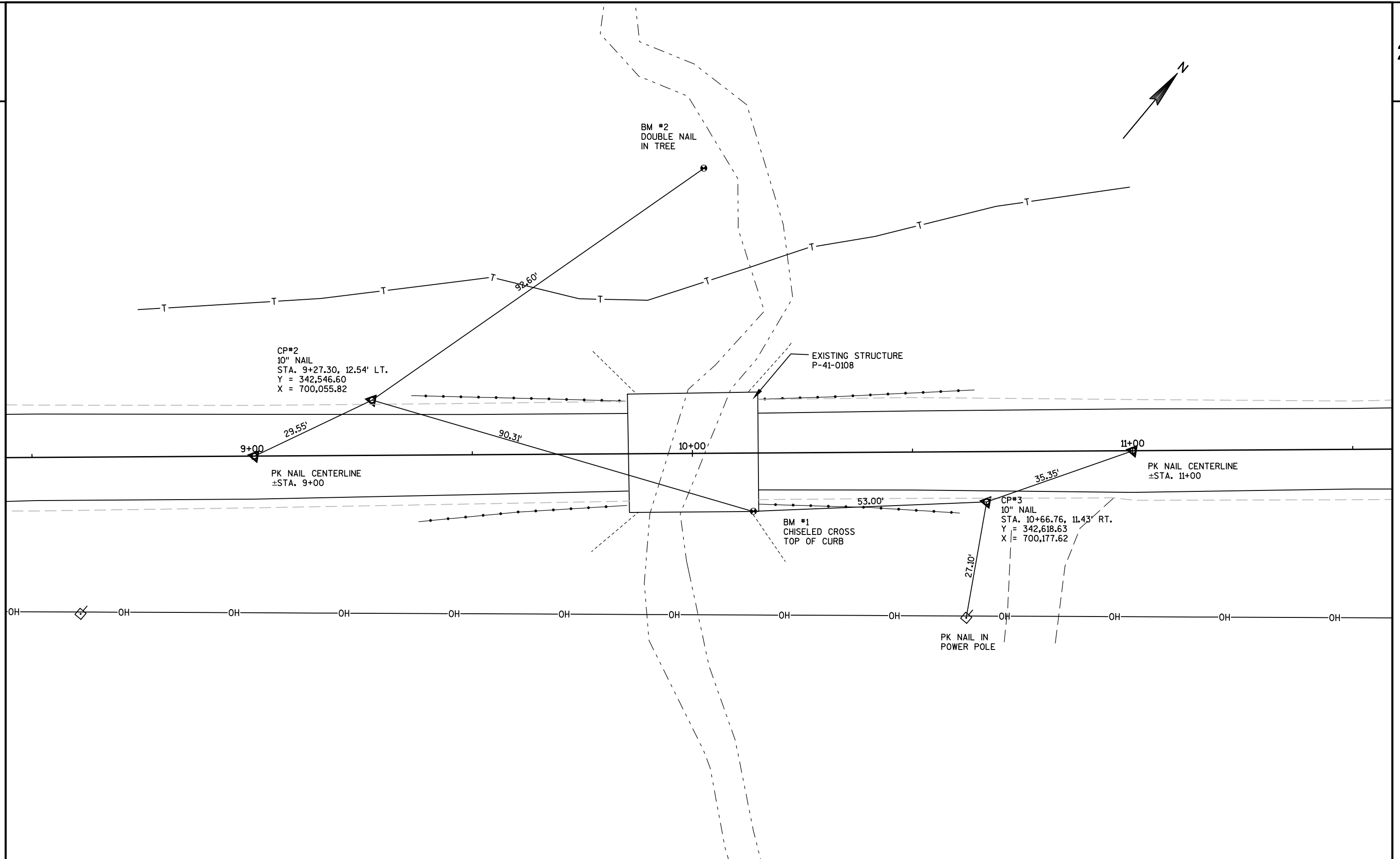
PI = 101+50.35
Y = 342,645.21
X = 700,047.01
 $\Delta = 35^\circ 14' 02''$ R
D = 572'57'28"
T = 3.18'
L = 6.15'
R = 10.00'
PC = 101+47.18
PT = 101+53.33

PI = 101+54.52
Y = 342,648.59
X = 700,044.24



CHANNEL REALIGNMENT LAYOUT





PROJECT NO:5119-00-72	HWY:CTH M	COUNTY:MONROE	CONTROL POINT TIES & COORDINATES	SHEET	E
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DATE 16AUG16		E S T I M A T E O F Q U A N T I T I E S			
LINE					5119-00-72
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	201.0205	Grubbing	STA	1.000	1.000
0020	203.0500.S	Removing Old Structure Over Waterway (station) 01. 10+00	LS	1.000	1.000
0040	205.0100	Excavation Common **P**	CY	70.000	70.000
0050	206.2000	Excavation for Structures Culverts (structure) 01. C-41-136	LS	1.000	1.000
0070	208.0100	Borrow	CY	589.000	589.000
0080	210.0100	Backfill Structure	CY	775.000	775.000
0090	213.0100	Finishing Roadway (project) 01. 5119-00-72	EACH	1.000	1.000
0110	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	645.000	645.000
0120	311.0115	Breaker Run	CY	160.000	160.000
0130	504.0100	Concrete Masonry Culverts	CY	126.000	126.000
0140	505.0400	Bar Steel Reinforcement HS Structures	LB	17,130.000	17,130.000
0150	516.0500	Rubberized Membrane Waterproofing	SY	17.000	17.000
0160	606.0300	Riprap Heavy	CY	185.000	185.000
0170	614.0920	Salvaged Rail	LF	191.000	191.000
0180	619.1000	Mobilization	EACH	0.434	0.434
0190	624.0100	Water	MGAL	63.000	63.000
0200	625.0500	Salvaged Topsoil	SY	1,315.000	1,315.000
0210	627.0200	Mulching	SY	1,690.000	1,690.000
0220	628.1504	Silt Fence	LF	750.000	750.000
0230	628.1520	Silt Fence Maintenance	LF	750.000	750.000
0240	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0250	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0260	628.2008	Erosion Mat Urban Class I Type B	SY	130.000	130.000
0270	629.0210	Fertilizer Type B	CWT	1.300	1.300
0280	630.0120	Seeding Mixture No. 20	LB	54.000	54.000
0290	630.0200	Seeding Temporary	LB	58.000	58.000
0300	633.5100	Markers Row	EACH	7.000	7.000
0310	633.5200	Markers Culvert End	EACH	4.000	4.000
0320	638.2602	Removing Signs Type II	EACH	4.000	4.000
0330	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0340	642.5001	Field Office Type B	EACH	0.440	0.440
0350	643.0100	Traffic Control (project) 01. 5119-00-72	EACH	1.000	1.000
0370	645.0105	Geotextile Type C	SY	285.000	285.000
0380	645.0120	Geotextile Type HR	SY	415.000	415.000
0390	650.4500	Construction Staking Subgrade	LF	275.000	275.000
0400	650.5000	Construction Staking Base	LF	275.000	275.000
0410	650.6500	Construction Staking Structure Layout (structure) 01. C-41-136	LS	1.000	1.000
0430	650.9910	Construction Staking Supplemental Control (project) 01. 5119-00-72	LS	1.000	1.000
0450	650.9920	Construction Staking Slope Stakes	LF	275.000	275.000
0470	715.0502	Incentive Strength Concrete Structures	DOL	756.000	756.000
0480	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	200.000	200.000
0490	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	400.000	400.000
0500	SPV.0105	Special 01. Temporary Water Diversion, Culvert C-41-136	LS	1.000	1.000
0510	SPV.0105	Special 02. Channel Excavation	LS	1.000	1.000
0520	SPV.0105	Special 03. Construction Staking, Channel Excavation	LS	1.000	1.000
0540	SPV.0180	Special 01. Relay Pulverized Material	SY	730.000	730.000

201.0205 GRUBBING

GRUBBING				
STATION	-	STATION	LOCATION	STA
10+00	-	11+00	LT	1
TOTALS:				1

205.0100 EXCAVATION COMMON **P**
208.0100 BORROW

LOCATION		EXC. COMMON CY	FILL CY (1)	EXPANDED FILL CY (2)
STA 8+75 - STA 9+72		40	252	328
STA 10+28 - STA 11+50		30	254	331
TOTALS:		70	506	659

(1) - NOT A BID ITEM - FOR INFORMATIONAL PURPOSES ONLY.
(2) - FILL EXPANSION 30%

305.0120 BASE AGGREGATE DENSE 1 1/4-INCH
624.0100 WATER

		1 1/4-INCH (2) TON	WATER (1) MGAL
STATION	- STATION		
8+75.00	- 11+50.00	625	12.5
10+81, RT F.E.		20	0.5
TOTALS:		645	13.0

(1) - ADDITIONAL QUANTITY INCLUDED WITH EROSION CONTROL AND RELAY PULVERIZED MATERIAL ITEMS.
(2) - QUANTITY ASSUMES 71 TONS OF PULVERIZED MATERIAL WILL BE REUSED IN THE BOTTOM LAYER OF THE PAVEMENT STRUCTURE.
ESTIMATED THICKNESS OF RE-LAID MATERIAL IS 1.8 INCHES AFTER MATERIAL IS SPREAD TO A WIDTH OF 30 FEET.

606.0300 RIPRAP HEAVY
645.0120 GEOTEXTILE FABRIC TYPE HR

LOCATION		RIPRAP CY	FABRIC SY
STREAM REALIGNMENT		48	131
TOTAL		48	131

614.0920 SALVAGED RAIL

STATION	-	STATION	LOCATION	RAIL LF
9+36	-	9+85	LT	49
9+38	-	9+85	RT	47
10+15	-	10+61	RT	46
10+15	-	10+64	LT	49
-				
TOTALS:				191

P - PAY PLAN QUANTITY

NOTE:
ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR
ENGINEER ESTIMATE CATEGORY 0010.

625.0500 SALVAGED TOPSOIL
627.0200 MULCHING
629.0210 FERTILIZER TYPE B
630.0120 SEEDING MIXTURE NO. 20
630.0200 SEEDING TEMPORARY
624.0100 WATER

			SALVAGED TOPSOIL SY	MULCHING SY	FERTILIZER CWT	SEEDING #20 LB	SEEDING TEMPORARY LB	WATER (1) MGAL
STATION	-	STATION	LOCATION					
8+75	-	11+50	LT	620	815	0.60	26	22
8+75	-	11+50	RT	575	720	0.55	23	19
STREAM REALIGNMENT STOCKPILE				-	-	-	4	3
UNDISTRIBUTED				120	155	0.15	5	4
TOTALS:				1315	1690	1.30	54	48

(1) - ADDITIONAL QUANTITY INCLUDED WITH BASE AGGREGATE AND RELAY PULVERIZED MATERIAL ITEMS.

628.1504 SILT FENCE
628.1520 SILT FENCE MAINTENANCE

			FENCE LF	MAINT. LF
STATION	-	STATION	LOCATION	
8+75	-	9+95	RT	125
9+50	-	9+80	LT	85
9+85	-	11+00	LT	190
10+05	-	10+65	RT	65
10+85	-	11+50	RT	65
STREAM REALIGNMENT STOCKPILE			150	150
UNDISTRIBUTED			70	70
TOTALS:			750	750

628.2008 EROSION MAT URBAN CLASS I TYPE B

		URBAN CLASS I TYPE B SY
LOCATION		
STA 8+75 - STA 9+60, LT		76
STA 10+35 - STA 10+80, LT		40
UNDISTRIBUTED		14
TOTALS:		130

628.1905 MOBILIZATIONS EROSION CONTROL
628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL

		MOBILIZATION EACH	EMERGENCY MOB. EACH
DESCRIPTION			
PROJECT 5119-00-72		2	2
TOTALS:		2	2

633.5100 MARKERS ROW

STATION	OFFSET	LOCATION	EACH
5+60.19	33.00	LT	1
6+03.70	33.00	RT	1
7+00.42	70.51	RT	1
8+15.92	33.00	LT	1
9+60.00	62.07	LT	1
14+03.05	55.00	RT	1
14+04.95	59.99	LT	1
TOTAL:			7

NOTE:
ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR
ENGINEER ESTIMATE CATEGORY 0010.

SPV.0180.01 RELAY PULVERIZED MATERIAL				
624.0100 WATER				
		RELAY PULVERIZED MATERIAL		WATER (1)
STATION	-	STATION	SY	MGAL
8+75	-	9+72	325	1
10+28	-	11+50	405	1
TOTALS:			730	2

(1) - ADDITIONAL QUANTITY INCLUDED WITH BASE AGGREGATE AND EROSION CONTROL ITEMS.

633.5200 MARKERS CULVERT END

STATION	LOCATION	EACH
9+88	29' RT	1
9+88	27' LT	1
10+12	29' RT	1
10+12	27' LT	1
TOTAL:		4

SPV.0105.02 CHANNEL EXCAVATION
SPV.0105.03 CONSTRUCTION STAKING, CHANNEL EXCAVATION

		CHANNEL EXCAVATION	CHANNEL STAKING
LOCATION		EACH	EACH
STREAM REALIGNMENT		1	1
TOTAL		1	1

638.2602 REMOVING SIGNS TYPE II
638.3000 REMOVING SMALL SIGN SUPPORTS

		REMOVING SIGNS TYPE II	REMOVING SMALL SIGN SUPPORTS	COMMENTS
STATION	LOCATION	EACH	EACH	
9+84	RT	1	1	EXISTING OBJECT MARKER
9+84	LT	1	1	EXISTING OBJECT MARKER
10+16	RT	1	1	EXISTING OBJECT MARKER
10+16	LT	1	1	EXISTING OBJECT MARKER
TOTALS:		4	4	

650.4500 CONSTRUCTION STAKING SUBGRADE
650.5000 CONSTRUCTION STAKING BASE
650.9920 CONSTRUCTION STAKING SLOPE STAKES
650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 5119-00-72

		SUBGRADE	BASE	SLOPE STAKES	SUPPLEMENTAL CONTROL
STATION	-	STATION	LF	LF	LS
8+75	-	11+50	275	275	-
TOTALS:			275	275	1

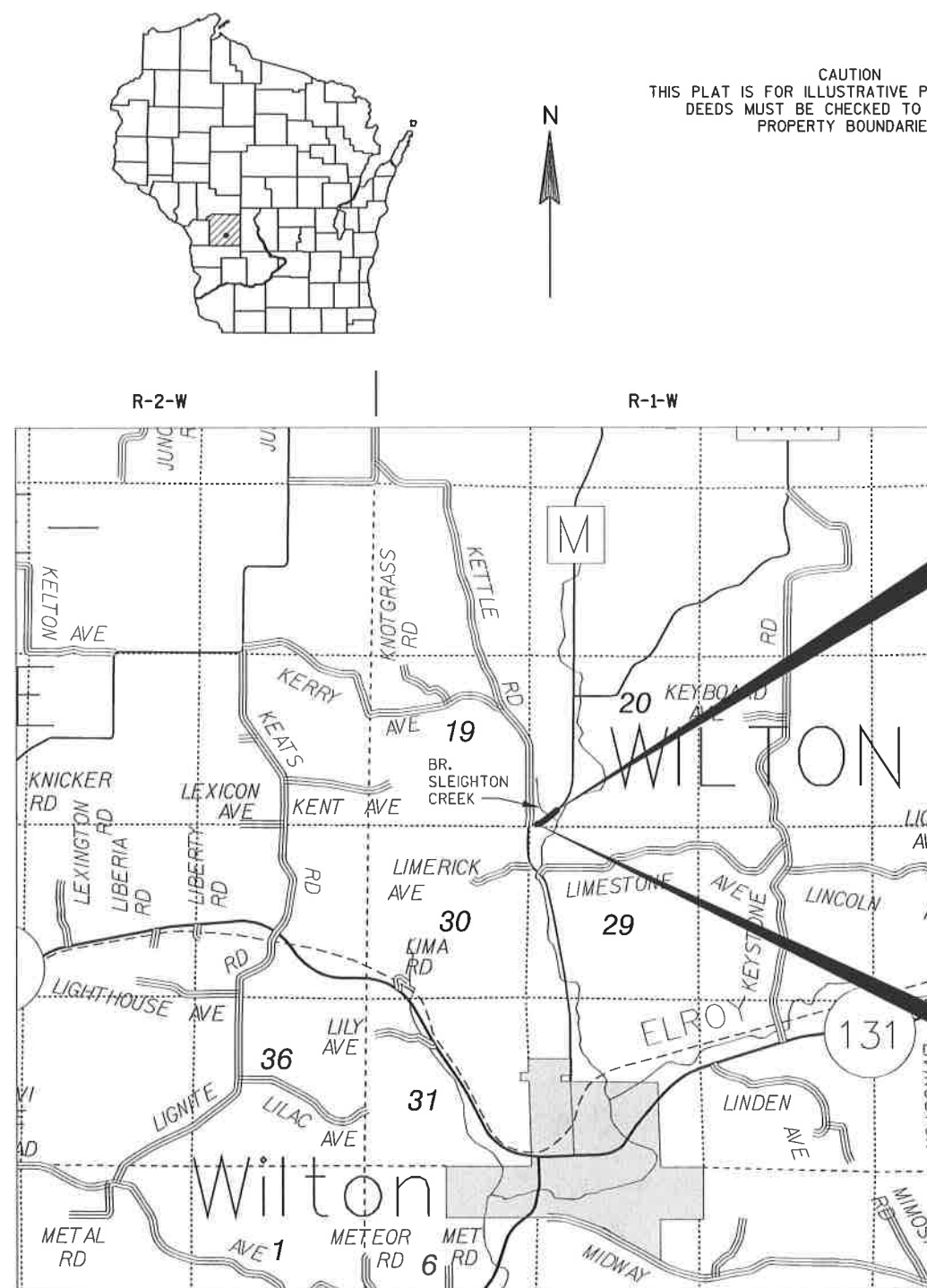
SECTION LINE		SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	
QUARTER LINE		SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	
SIXTEENTH LINE		GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	
NEW REFERENCE LINE		SIXTEENTH CORNER MONUMENT			
NEW R/W LINE		SIGN		OFF-PREMISE SIGN	
EXISTING R/W OR HE LINE					
PROPERTY LINE					
LOT, TIE & OTHER MINOR LINES					
SLOPE INTERCEPT					
CORPORATE LIMITS					
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)		ELECTRIC POLE		COMPENSABLE	NON-COMPENSABLE
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)		TELEPHONE POLE			
TEMPORARY LIMITED EASEMENT AREA		PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)					
TRANSMISSION STRUCTURES		ACCESS RESTRICTED BY ACQUISITION			
BUILDING TO BE REMOVED		NO ACCESS (BY STATUTORY AUTHORITY)			
BRIDGE		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
		NO ACCESS (NEW HIGHWAY)			
		PARCEL NUMBER		UTILITY NUMBER	
		PARALLEL OFFSETS			

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI	T-16-N
ACRES	AC	PROPERTY LINE	PL	
AHEAD	AH	RECORDED AS	(100')	
ALUMINUM	ALUM	REEL / IMAGE	R/I	
AND OTHERS	ET AL	REFERENCE LINE	R/L	
BACK	BK	REMAINING	REM	
BLOCK	BLK	RESTICTIVE DEVELOPMENT	RDE	
CENTERLINE	C/L	EASEMENT		
CERTIFIED SURVEY MAP	CSM	RIGHT	RT	
CONCRETE	CONC	RIGHT OF WAY	R/W	
COUNTY	CO	SECTION	SEC	
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV	
DISTANCE	DIST	SQUARE FEET	SF	
CORNER	COR	STATE TRUNK HIGHWAY	STH	
DOCUMENT NUMBER	DOC	STATION	STA	
EASEMENT	EASE	TELEPHONE PEDESTAL	TP	
EXISTING	EX	TEMPORARY LIMITED	TLE	
GAS VALVE	GV	EASEMENT		
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP	
HIGHWAY EASEMENT	HE	PLAT		
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH	
LAND CONTRACT	LC	VOLUME	V	
LEFT	LT			
MONUMENT	MON			
NATIONAL GEODETIC SURVEY	NGS			
NUMBER	NO			
OUTLOT	OL	LONG CHORD	LCH	
PAGE	P	LONG CHORD BEARING	LCB	
POINT OF TANGENCY	PT	RADIUS	R	
PERMANENT LIMITED	PLE	DEGREE OF CURVE	D	
EASEMENT		CENTRAL ANGLE	Δ/DELTA	
POINT OF BEGINNING	POB	LENGTH OF CURVE	L	
POINT OF CURVATURE	PC	TANGENT	T	
POINT OF COMPOUND CURVE	PCC	DIRECTION AHEAD	DA	
		DIRECTION BACK	DB	

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES,
MONROE COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE
GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID
DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT OF WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER
OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR
OTHER SURVEYS OF PUBLIC RECORD.

WATER — W —
GAS — G —
TELEPHONE — T —
OVERHEAD — OH —
TRANSMISSION LINES
ELECTRIC — E —
CABLE TELEVISION — TV —
FIBER OPTIC — FO —
SANITARY SEWER — SAN —
STORM SEWER — SS —



CAUTION
THIS PLAT IS FOR ILLUSTRATIVE PURPOSES ONLY.
DEEDS MUST BE CHECKED TO DETERMINE
PROPERTY BOUNDARIES.

END RELOCATION
ORDER STA. 14+03.92

$$\begin{aligned} Y &= 342,847.22 \\ X &= 700,425.76 \end{aligned}$$

506.85 FEET NORTH OF AND 908.58 FEET
EAST OF THE SOUTHWEST CORNER OF SECTION
20, T-16-N, R-1-W, TOWN OF WILTON, MONROE
COUNTY, WI

BEGIN RELOCATION
ORDER STA. 5+39.24

$$\begin{aligned} Y &= 342,361.75 \\ X &= 699,727.17 \end{aligned}$$

2138 FEET NORTH OF AND 209.99 FEET EAST
OF THE SOUTHWEST CORNER OF SECTION 20,
T-16-N, R-1-W, TOWN OF WILTON, MONROE
COUNTY, WI

LAYOUT
 SCALE 0 1 MILE
 TOTAL NET LENGTH OF CENTERLINE = 0.164

R/W PROJECT NUMBER 5119-00-02	SHEET NUMBER	TOTAL SHEETS
R/W PROJECT NUMBER	4.01	2
PLAT OF RIGHT OF WAY REQUIRED FOR WILTON - TOMAH (BR SLEIGHTON CREEK BRIDGE C-41-0136) CTH M		
		MONROE COUNTY
CONSTRUCTION PROJECT NUMBER: 5119-00-72		

RECEIVED
FEB 26 2016

MONROE COUNTY CLERK

ORIGINAL PLAT PREPARED BY

MSA**PROFESSIONAL SERVICES**

**TRANSPORTATION • MUNICIPAL
DEVELOPMENT • ENVIRONMENTAL**
1230 South Boulevard Baraboo, WI 53913
608-356-2771 1-800-362-4505 Fax: 608-356-2770
Web Address: www.msa-ps.com
© MSA Professional Services, Inc.



1/12/2016
(Date)

Angelo P. Rhinides
(Professional Land Surveyor)

REVISION DATE

APPROVED FOR MONROE COUNTY

DATE: 726/16 JUNE 1966

FILE NAME : P:\5800S\5840S\5848\05848016\CADD\RW\5848016 PLAT TS.DWG
LAYOUT NAME - RW TITLE SHEET

PLOT DATE : 1/12/2016 9:30 AM

PLOT BY : BARB HALLEY

PLOT NAME :

WISDOT/CADDS SHEET 50

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE COUNTY.

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES
			NEW	EXISTING	TOTAL	
1	SAMUEL I. BELL AND COLLEEN J. BELL	FEE & TLE	0.28	--	0.28	0.09
2	RANDY J. MORRIS AND PATRICIA L. MORRIS	FEE	0.34	1.00	1.34	--
98	OAKDALE ELECTRIC COOPERATIVE	RELEASE OF RIGHTS	--	--	--	--
99	CENTURYLINK	RELEASE OF RIGHTS	--	--	--	--

CURVE TABLE				
CURVE #	LENGTH	CHORD LENGTH	BEARING	RADIUS
C1	236.37'	233.00'	N66° 27' 05"E	403.00'

EXISTING HIGHWAY R/W ESTABLISHED FROM RECORDED SURVEY MAPS, RIGHT OF WAY PLAT PROJECT #2451-2003, EXISTING IRONS LOCATED IN THE FIELD AND EXISTING SECTION CORNERS.

NOTE: INVERSING BETWEEN COORDINATES, IN CLOSE PROXIMITY WITH EACH OTHER, MAY NOT REPLICATE THE BEARINGS AND DISTANCES SHOWN ON THIS PLAT.

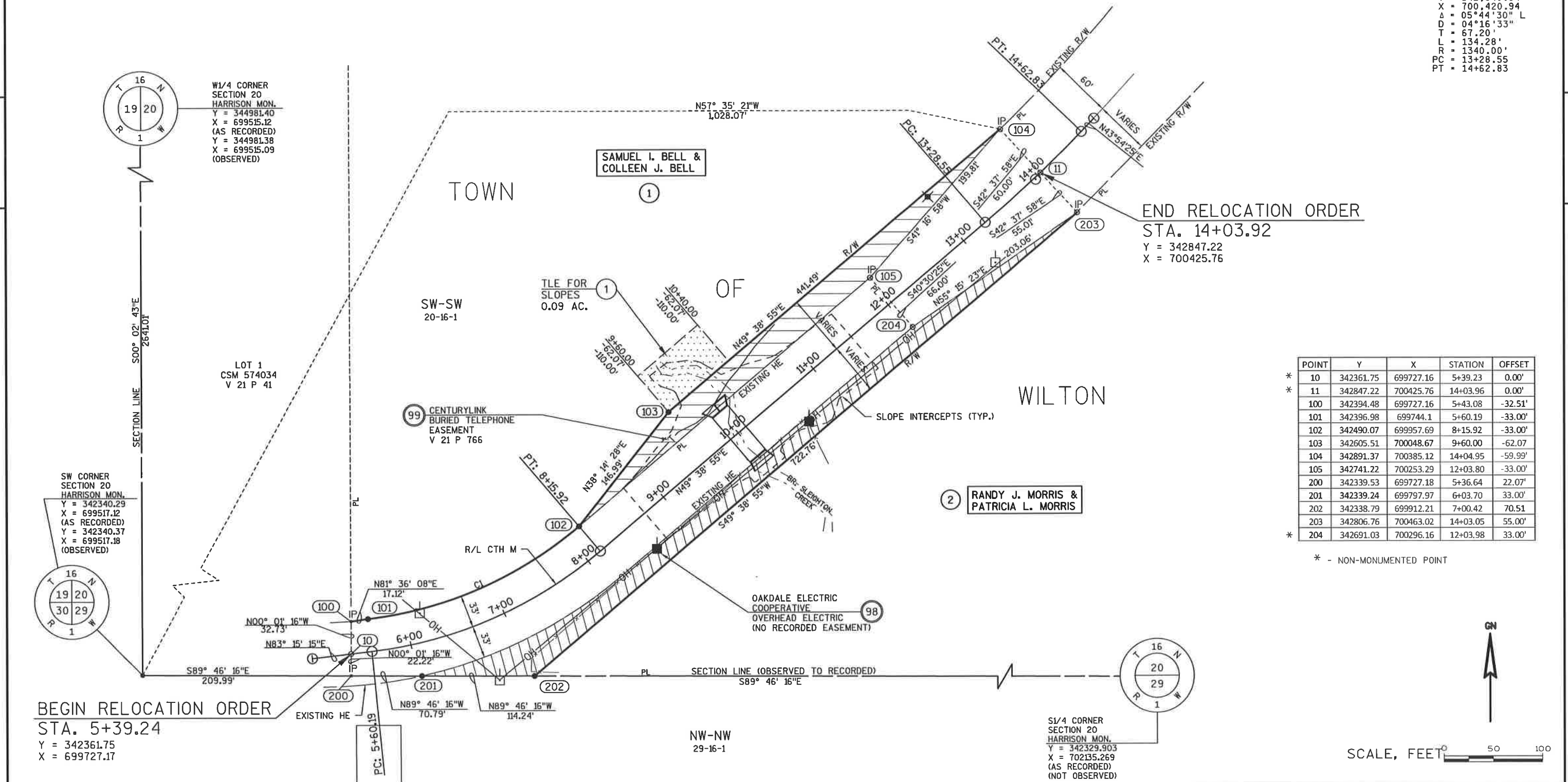
ALIGNMENT DATA

PI = 6+91.85
Y = 342,379.68
X = 699,878.72
Δ = 33°36'20" L
D = 13°08'28"
T = 131.66'
L = 255.73'
R = 436.00'
PC = 5+60.19
PT = 8+15.92

PI = 13+95.69
Y = 342,840.34
X = 700,420.94
Δ = 05°44'30" L
D = 04°16'33"
T = 67.20'
L = 134.28'
R = 1340.00'
PC = 13+28.55
PT = 14+62.83

4

4



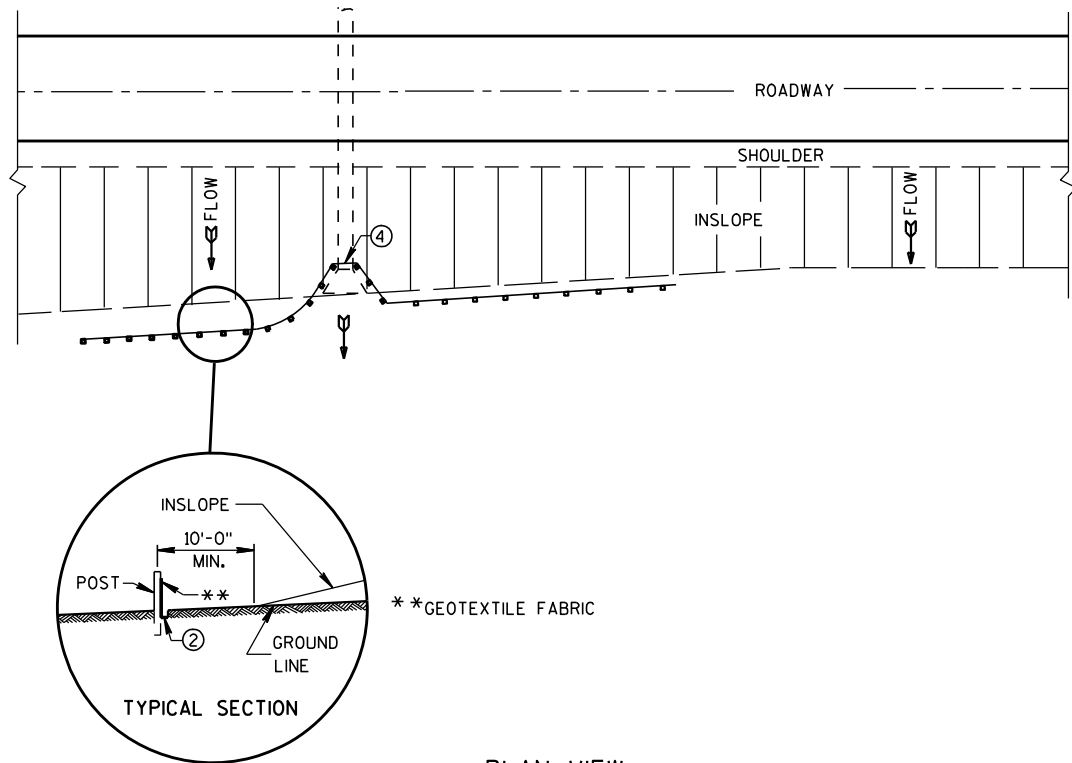
POINT	Y	X	STATION	OFFSET
* 10	342361.75	699727.16	5+39.23	0.00'
* 11	342847.22	700425.76	14+03.96	0.00'
100	342394.48	699727.16	5+43.08	-32.51'
101	342396.98	699744.1	5+60.19	-33.00'
102	342490.07	699957.69	8+15.92	-33.00'
103	342605.51	700048.67	9+60.00	-62.07'
104	342891.37	700385.12	14+04.95	-59.99'
105	342741.22	700253.29	12+03.80	-33.00'
200	342339.53	699727.18	5+36.64	22.07'
201	342339.24	699797.97	6+03.70	33.00'
202	342338.79	699912.21	7+00.42	70.51'
203	342806.76	700463.02	14+03.05	55.00'
* 204	342691.03	700296.16	12+03.98	33.00'

* - NON-MONUMENTED POINT

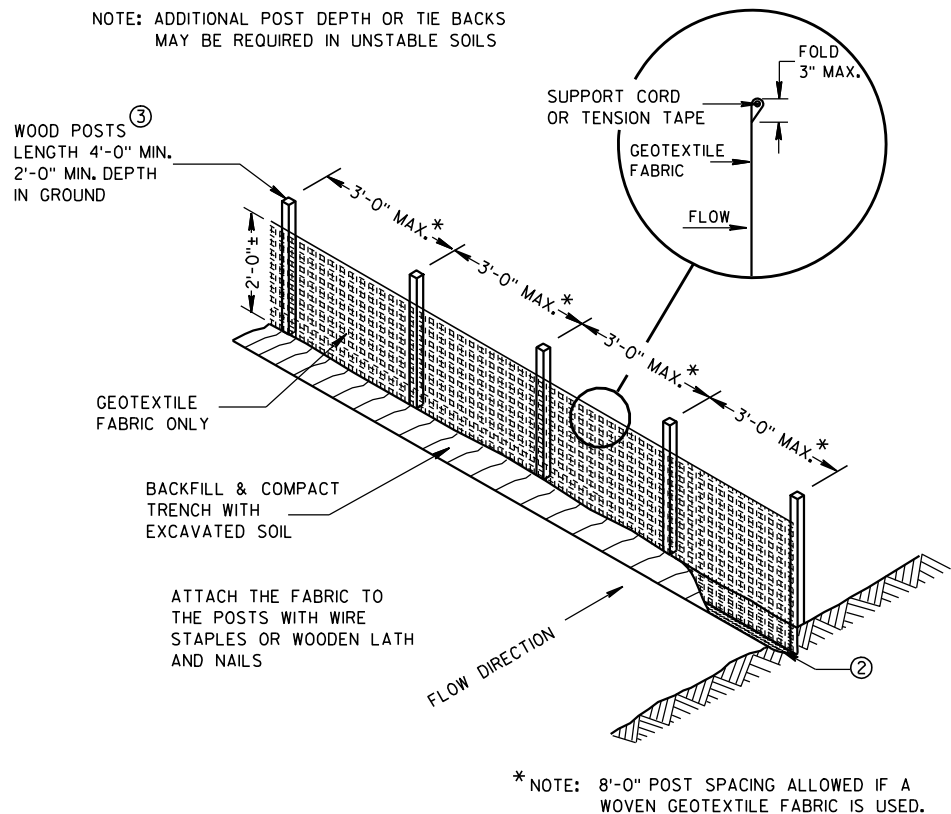
REVISION DATE	DATE : 1/12/2016	SCALE, FEET	HWY: CTH M	STATE R/W PROJECT NUMBER 5119-00-02	PLAT SHEET 4.02
	GRID FACTOR N/A	0 50 100	COUNTY: MONROE	CONSTRUCTION PROJECT NUMBER 5119-00-72	PS&E SHEET E

Standard Detail Drawing List

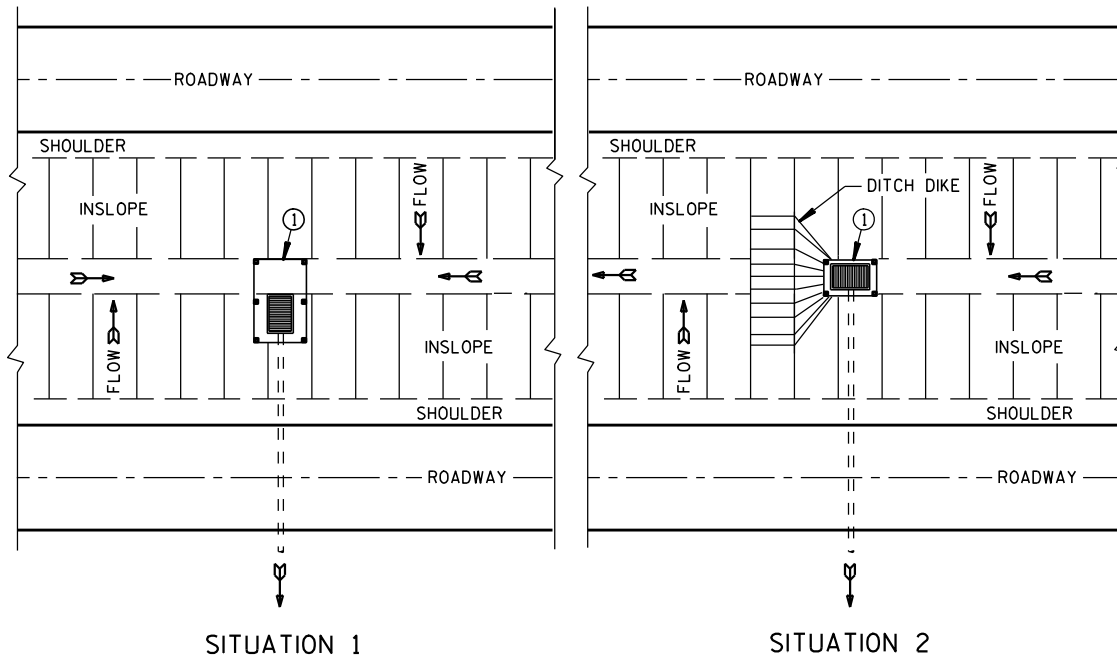
08E09-06	SILT FENCE
12A03-10	NAME PLATE (STRUCTURES)
15A01-12A	MARKER POST FOR RIGHT-OF-WAY
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES



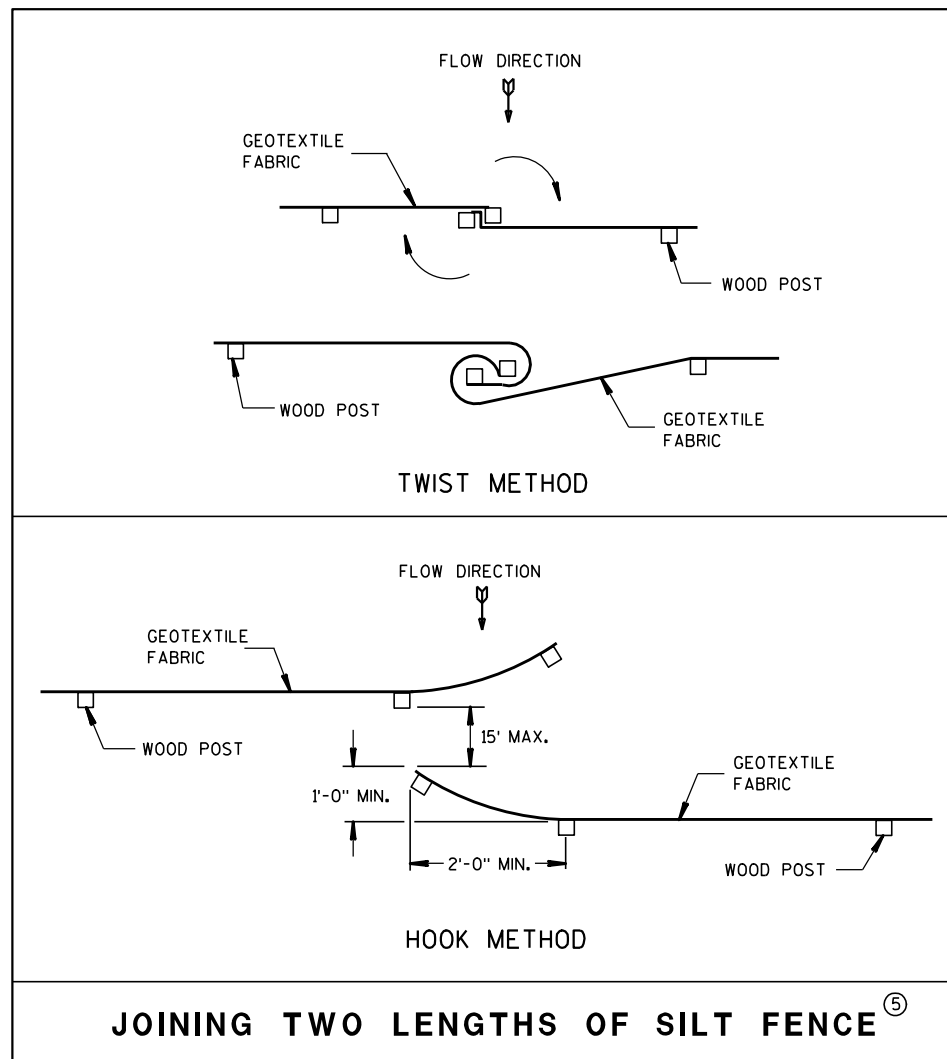
PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE



SILT FENCE



PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

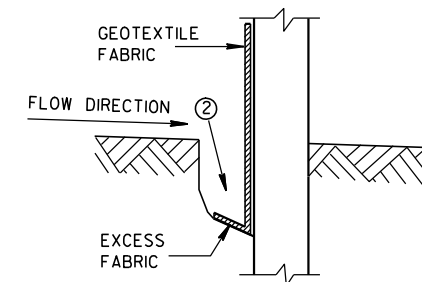


JOINING TWO LENGTHS OF SILT FENCE ⑤

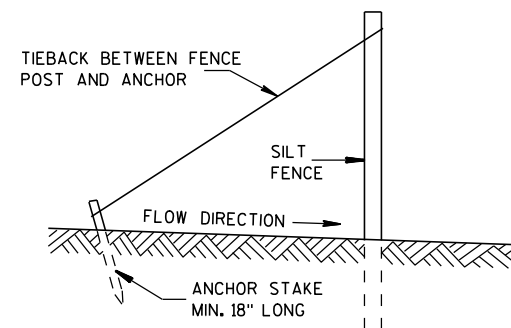
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

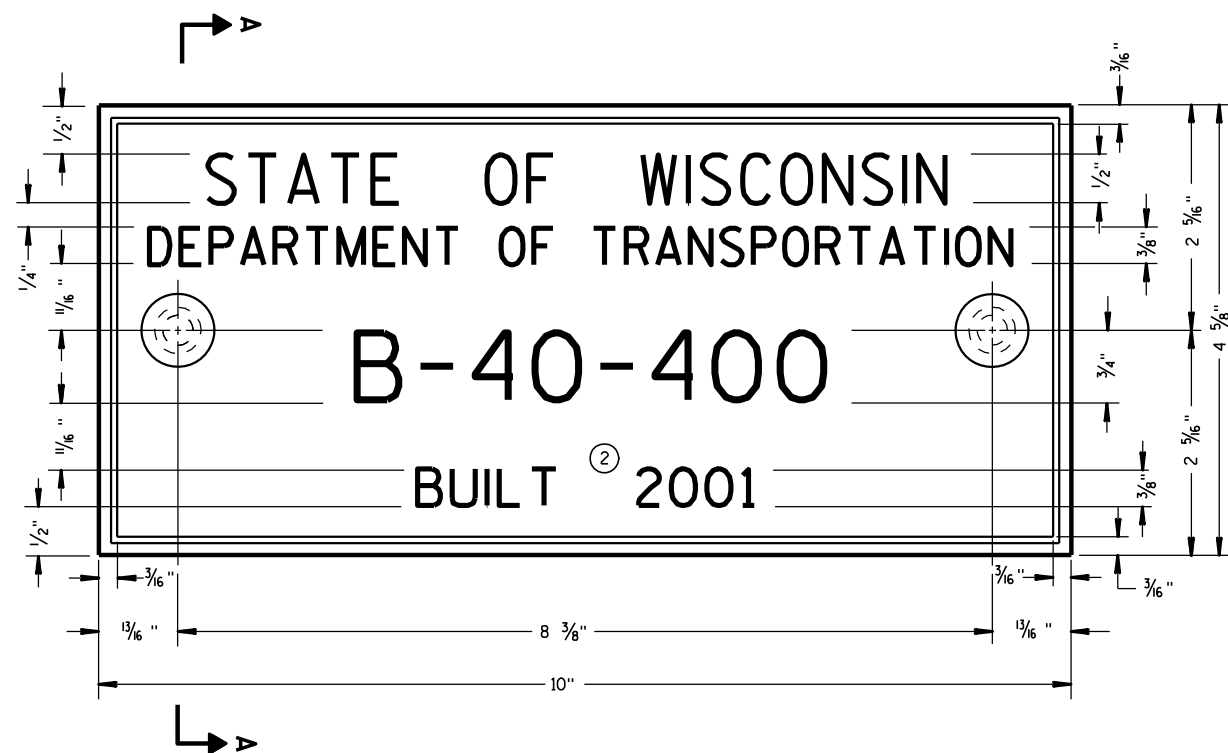


TRENCH DETAIL

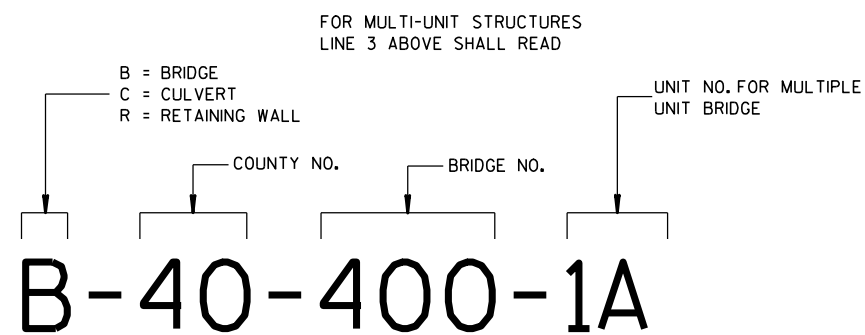


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)



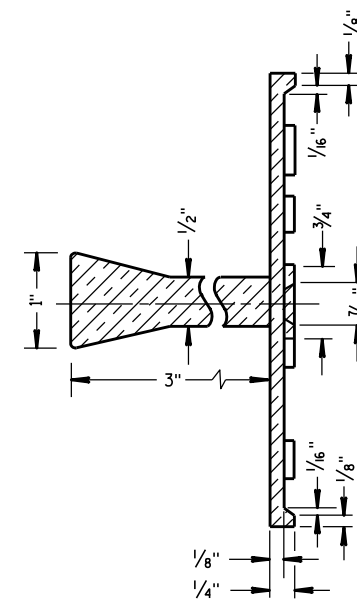
**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

GENERAL NOTES

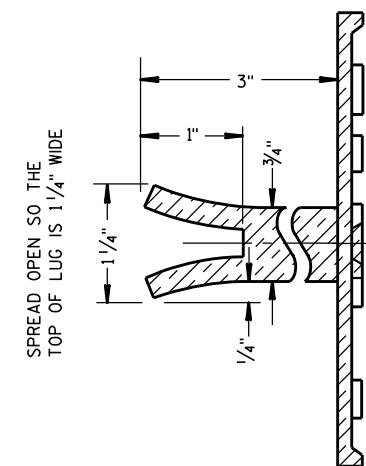
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

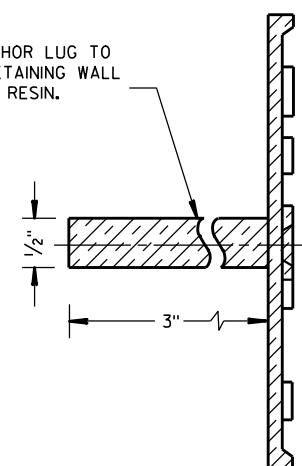


SECTION A-A



ALTERNATE LUG

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

**NAME PLATE
(STRUCTURES)**

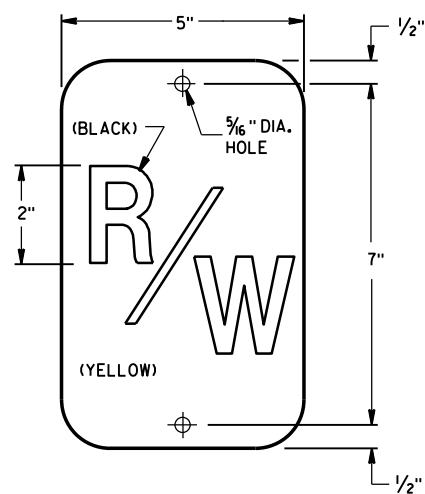
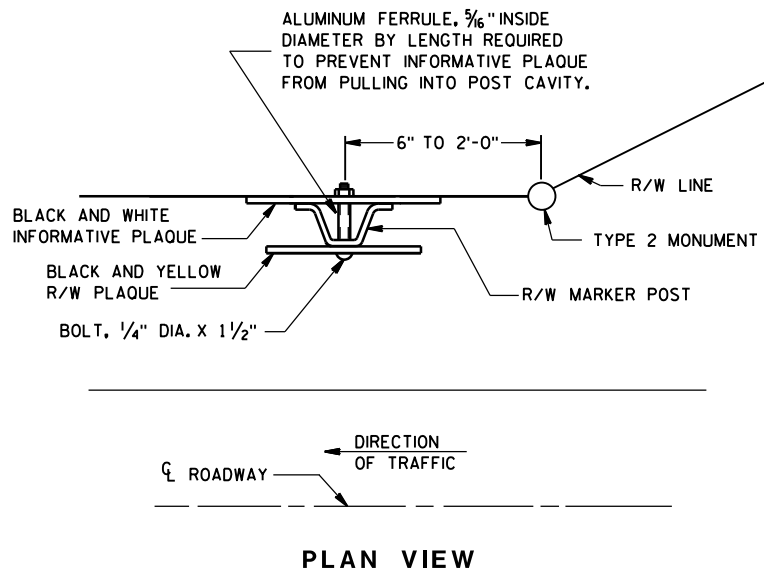
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

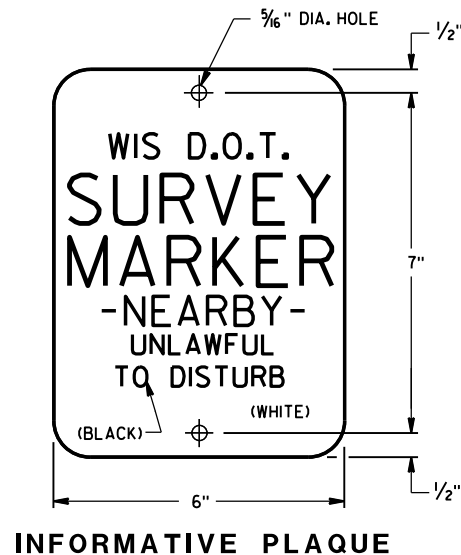
3/26/10
DATE

FHWA

/S/ Scot Becker
CHIEF STRUCTURAL DEVELOPMENT ENGINEER



THE RIGHT-OF-WAY PLAQUE AND INFORMATIVE PLAQUE WILL BE FURNISHED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION.



GENERAL NOTES

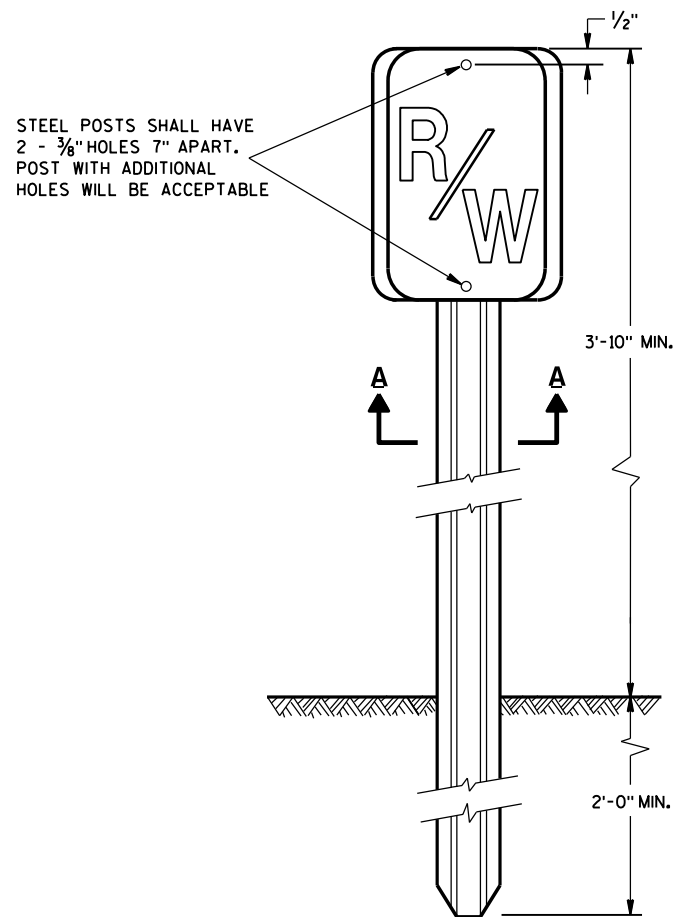
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

A STEEL MARKER POST FOR RIGHT-OF-WAY SHALL BE PLACED IN THE RIGHT-OF-WAY, WITH THE BACK OF THE POST ON THE LONGER RIGHT-OF-WAY TANGENT, 6 INCHES TO 24 INCHES FROM EACH TYPE 2 MONUMENT TO SERVE AS A GUARD POST, AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

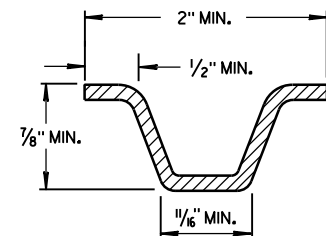
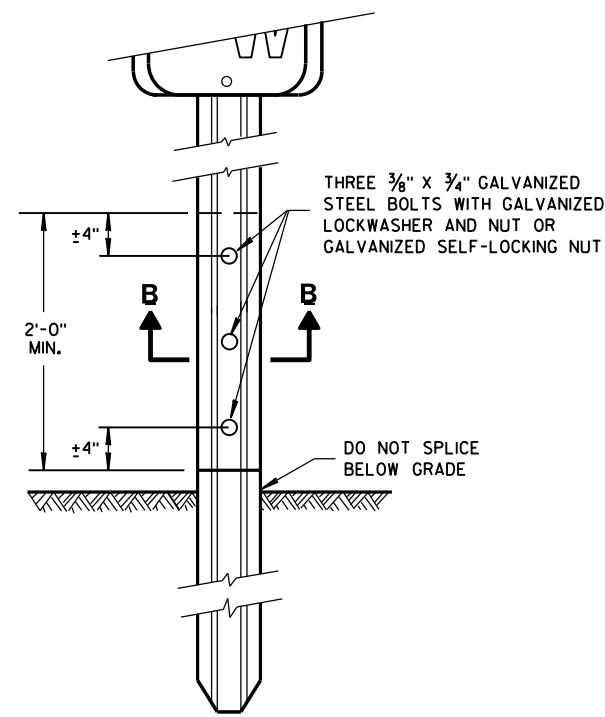
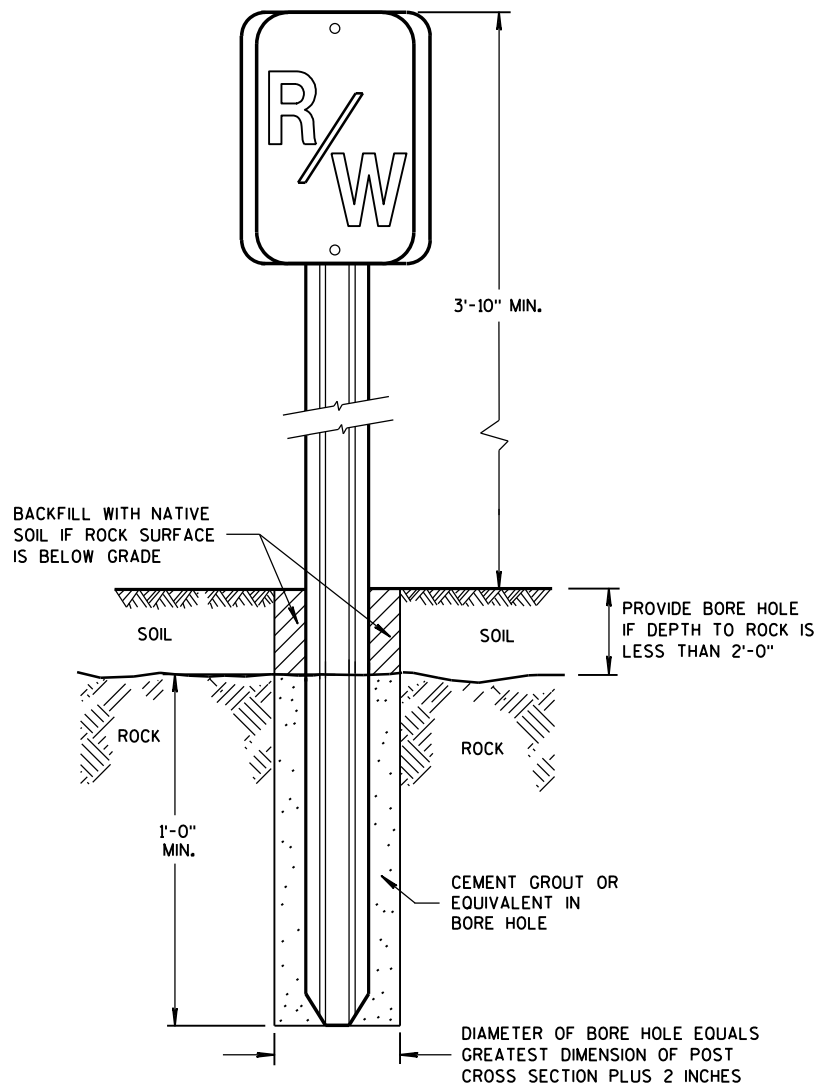
THE "R/W" PLAQUE SHALL FACE THE ROADWAY AND THE INFORMATIVE PLAQUE SHALL FACE AWAY FROM THE ROADWAY. R/W AND INFORMATIVE PLAQUES WILL BE FURNISHED BY THE DEPARTMENT OF TRANSPORTATION.

STEEL MARKER POSTS SHALL MEET THE MINIMUM MATERIAL REQUIREMENTS FOR STEEL DELINEATOR POSTS; EXCEPT POSTS PAINTED WITH FEDERAL YELLOW ENAMEL NEED NOT BE ZINC COATED.

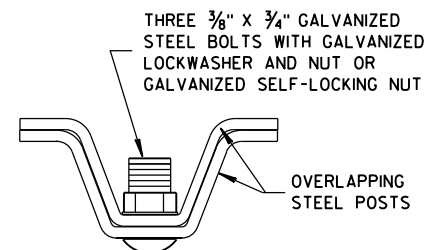
- ① IN AREAS OF SOLID ROCK, DRILL A BORE HOLE 2" GREATER THAN THE WIDEST DIMENSION OF THE POST CROSS SECTION INTO THE ROCK TO A MINIMUM DEPTH OF 12 INCHES. CUT OR SPLICE THE POST SO THAT A MINIMUM LENGTH OF 3' 10" PROTRUDES ABOVE THE GROUND. BLOW OUT THE BORE HOLE IN THE ROCK USING COMPRESSED AIR. FILL THE BORE HOLE WITH CEMENT GROUT, OR EQUIVALENT, DEPENDING ON THE STABILITY OF THE ROCK.



STEEL POSTS SHALL HAVE 2 - 5/8" HOLES 7" APART. POST WITH ADDITIONAL HOLES WILL BE ACCEPTABLE



MIN. WEIGHT 1.12 LB./FT.
SECTION A-A



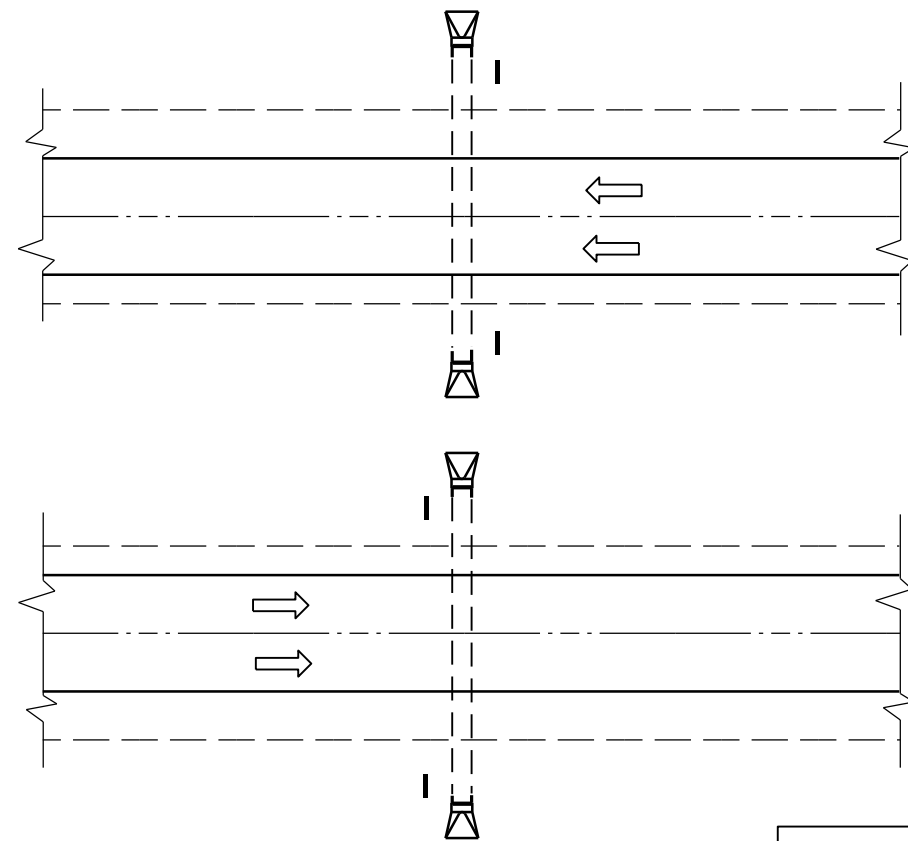
SECTION B-B

**MARKER POST
FOR RIGHT-OF-WAY**

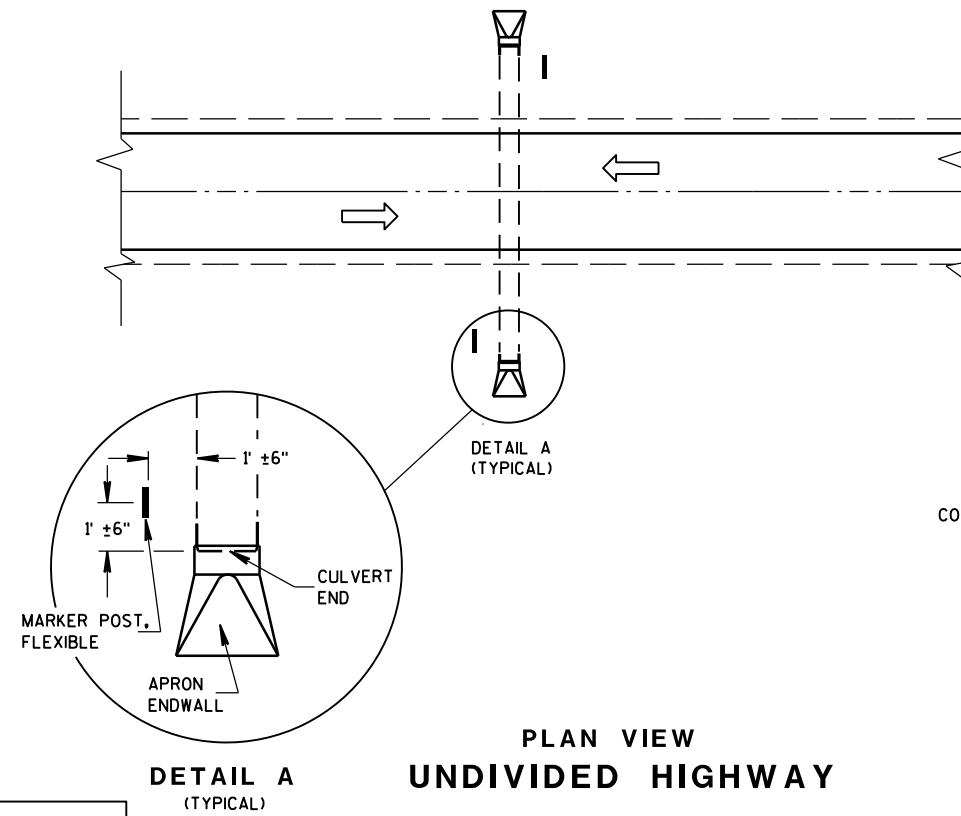
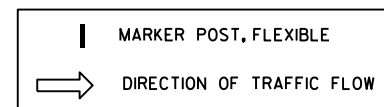
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June, 2015 /S/ Ray Kumapayi
DATE CHIEF SURVEYING AND MAPPING ENGINEER
FHWA



PLAN VIEW
DIVIDED HIGHWAY

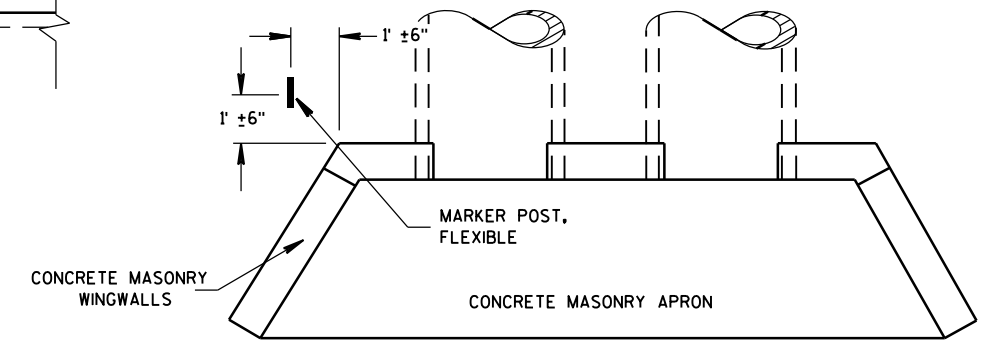


PLAN VIEW
UNDIVIDED HIGHWAY

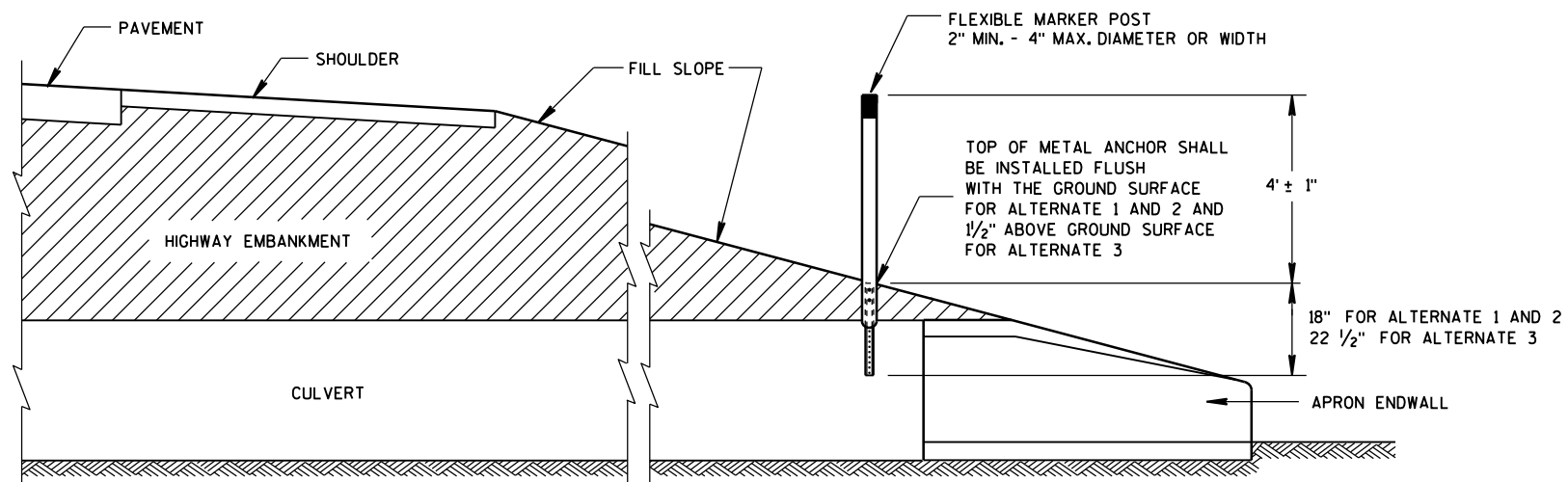
FLEXIBLE MARKER POST LOCATION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



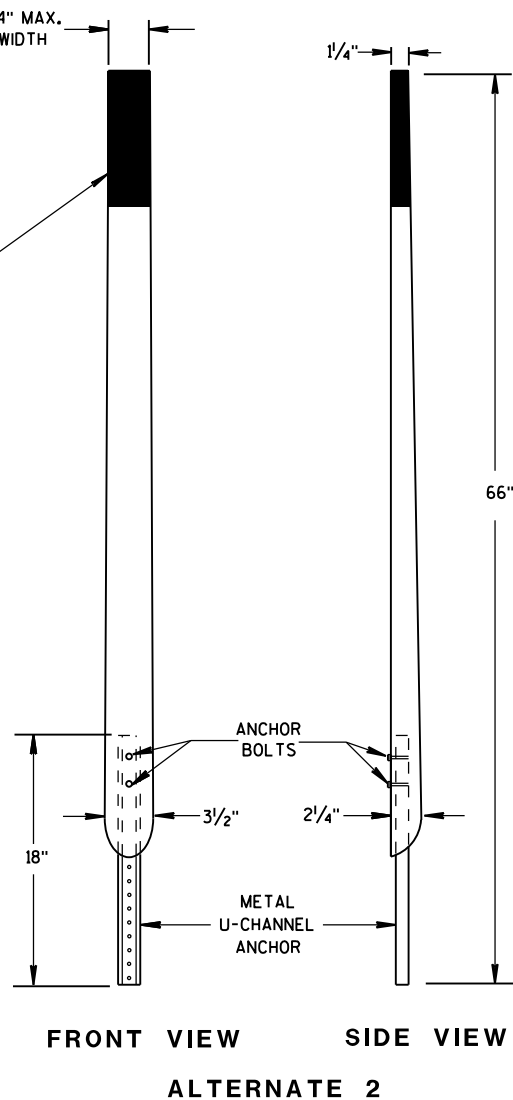
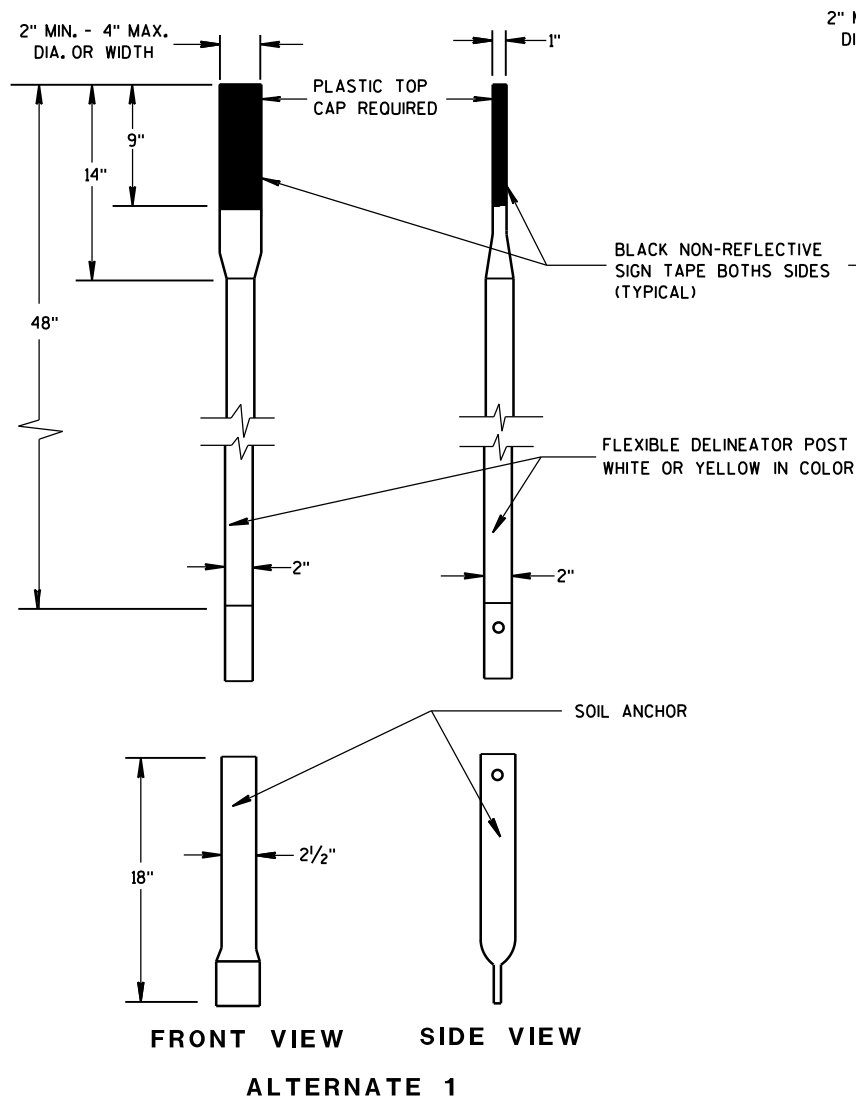
PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH



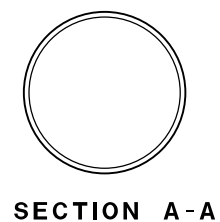
CROSS SECTION
FLEXIBLE MARKER POST

FLEXIBLE MARKER POST
FOR CULVERT END

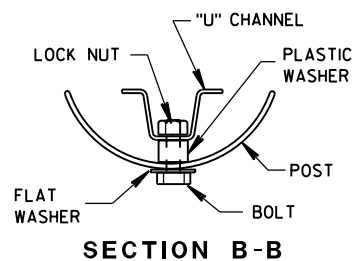
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



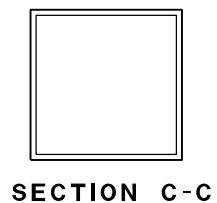
FLEXIBLE MARKER POSTS



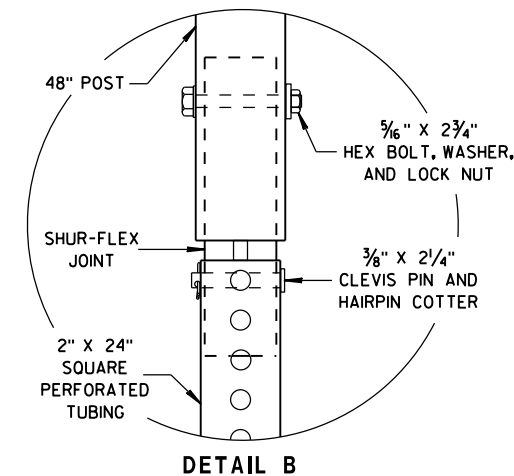
SECTION A-A



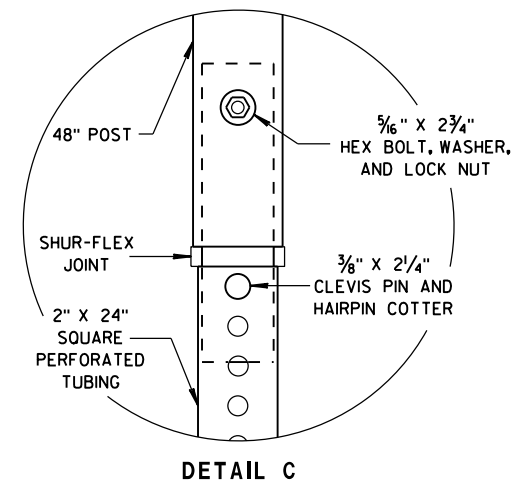
SECTION B-B



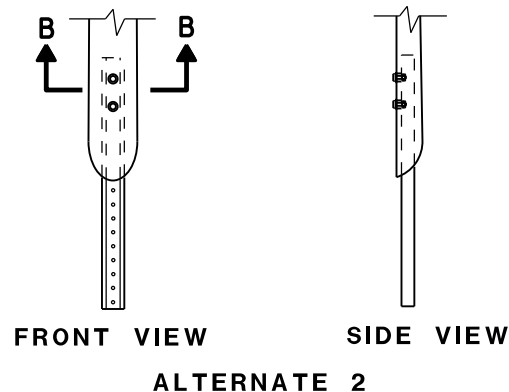
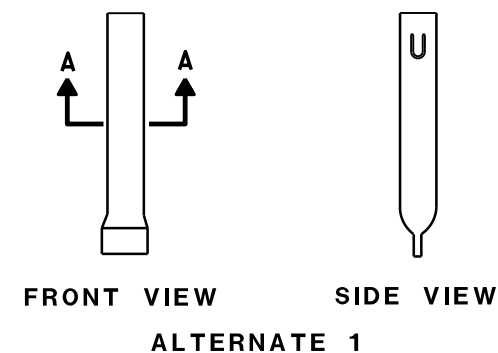
SECTION C-C



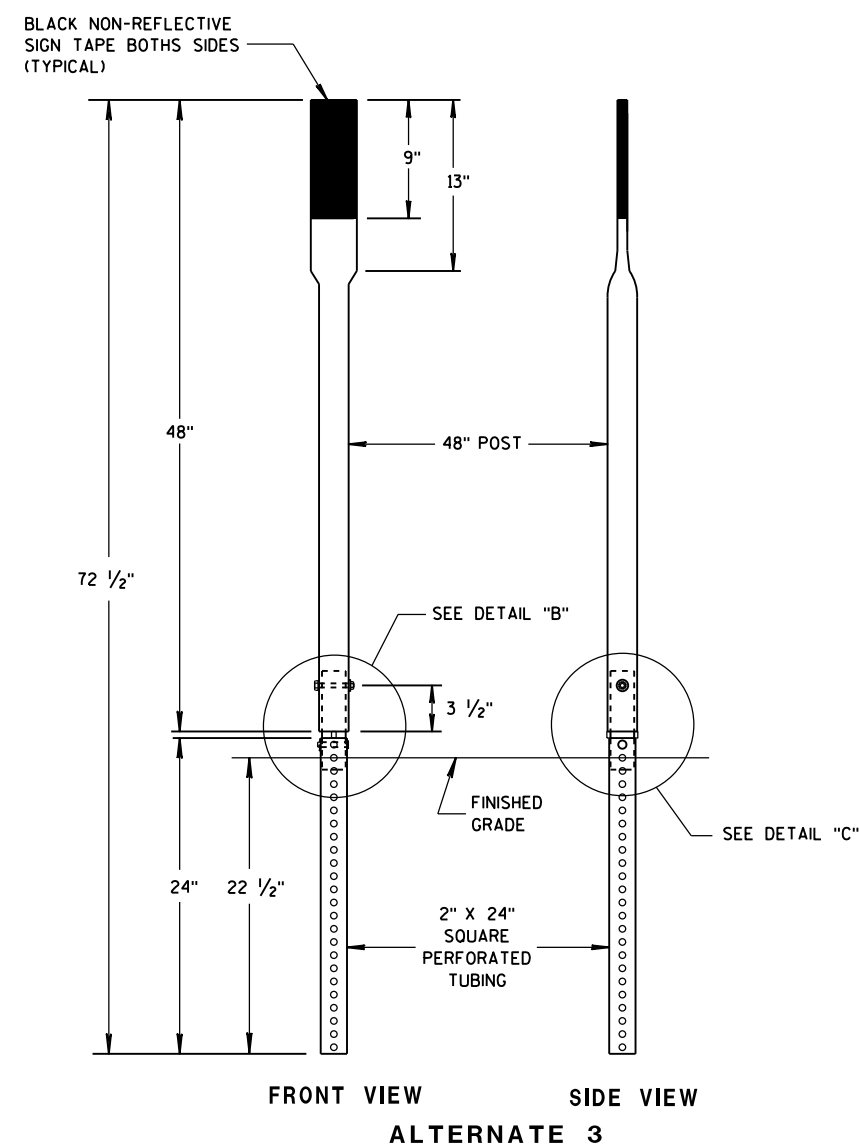
DETAIL B



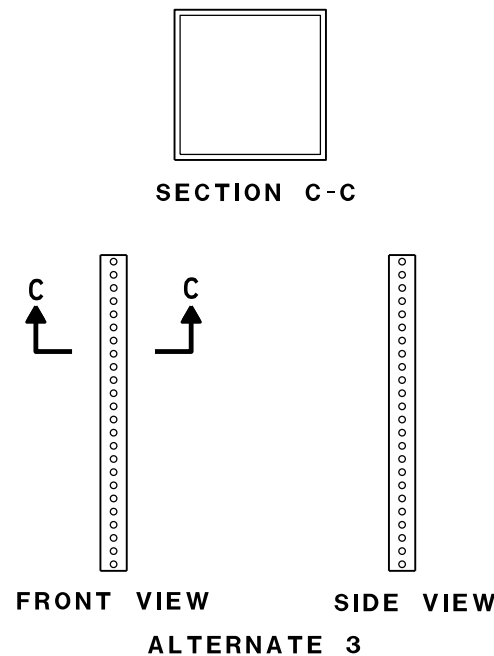
DETAIL C



FLEXIBLE MARKER POST ANCHORS

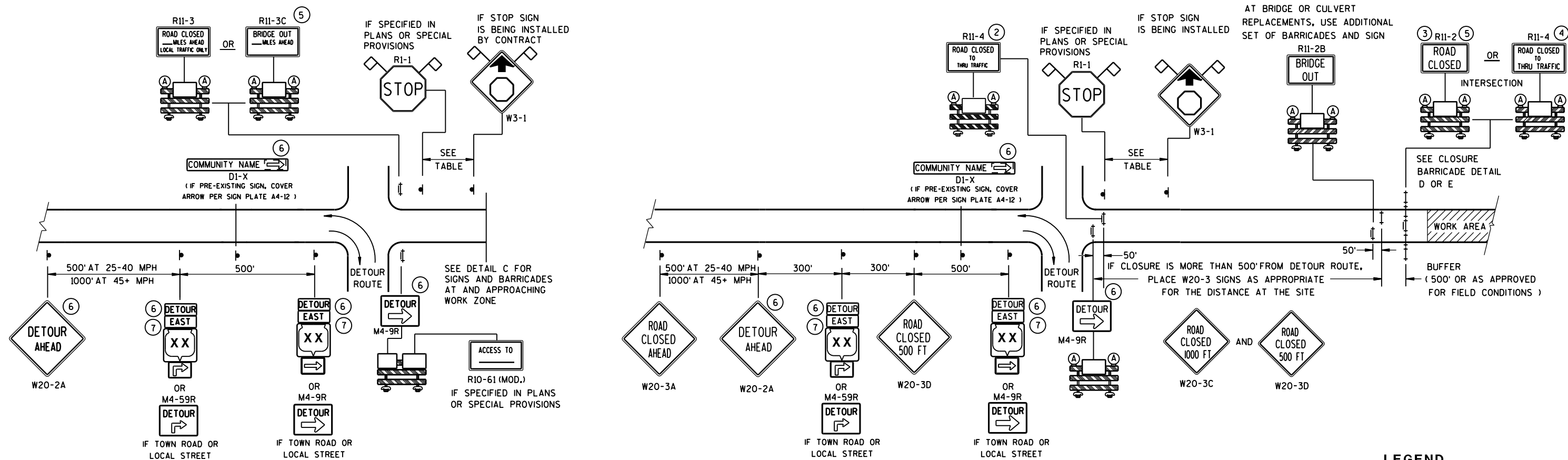


FLEXIBLE MARKER POSTS



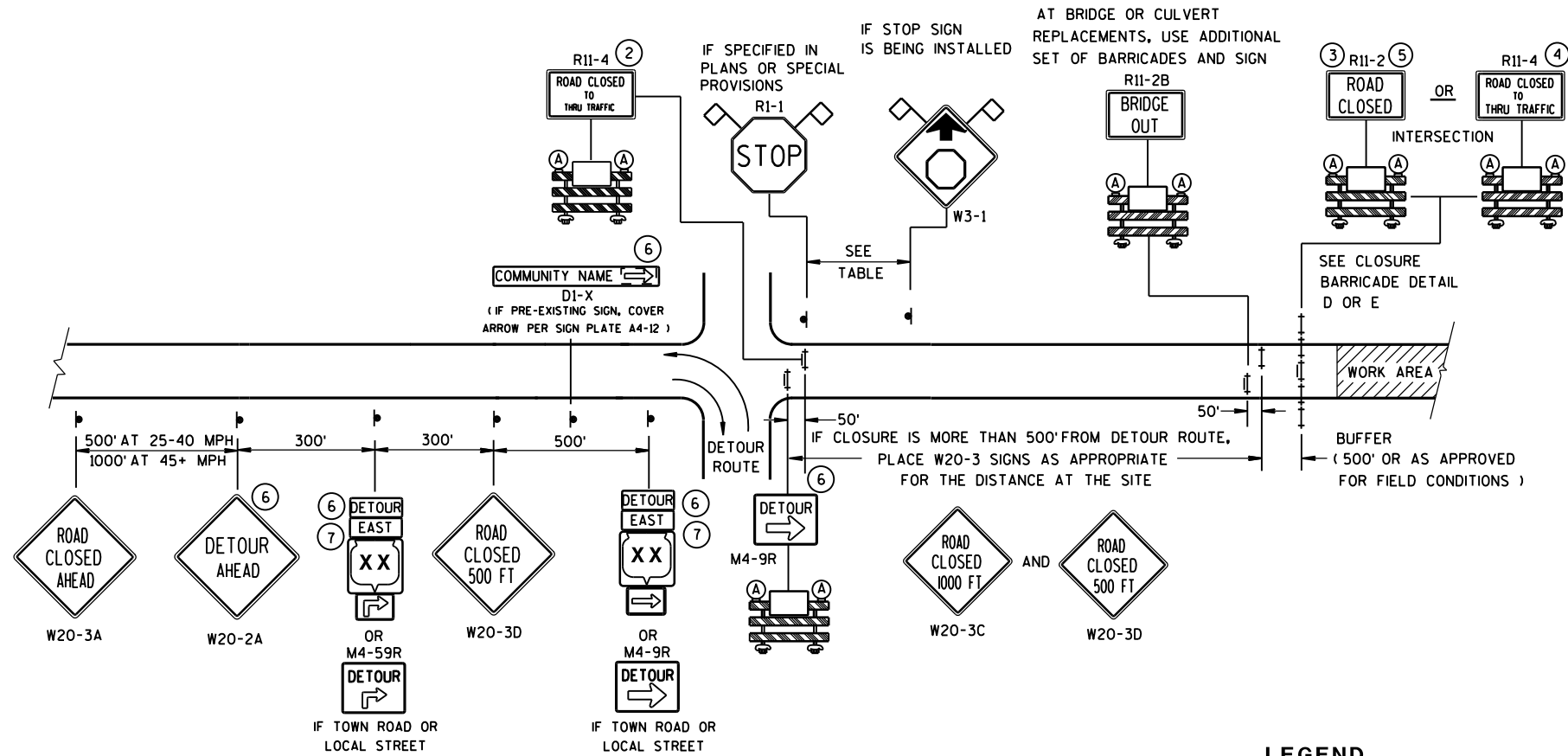
FLEXIBLE MARKER POSTS

FLEXIBLE MARKER POST FOR CULVERT END	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/1/2012 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



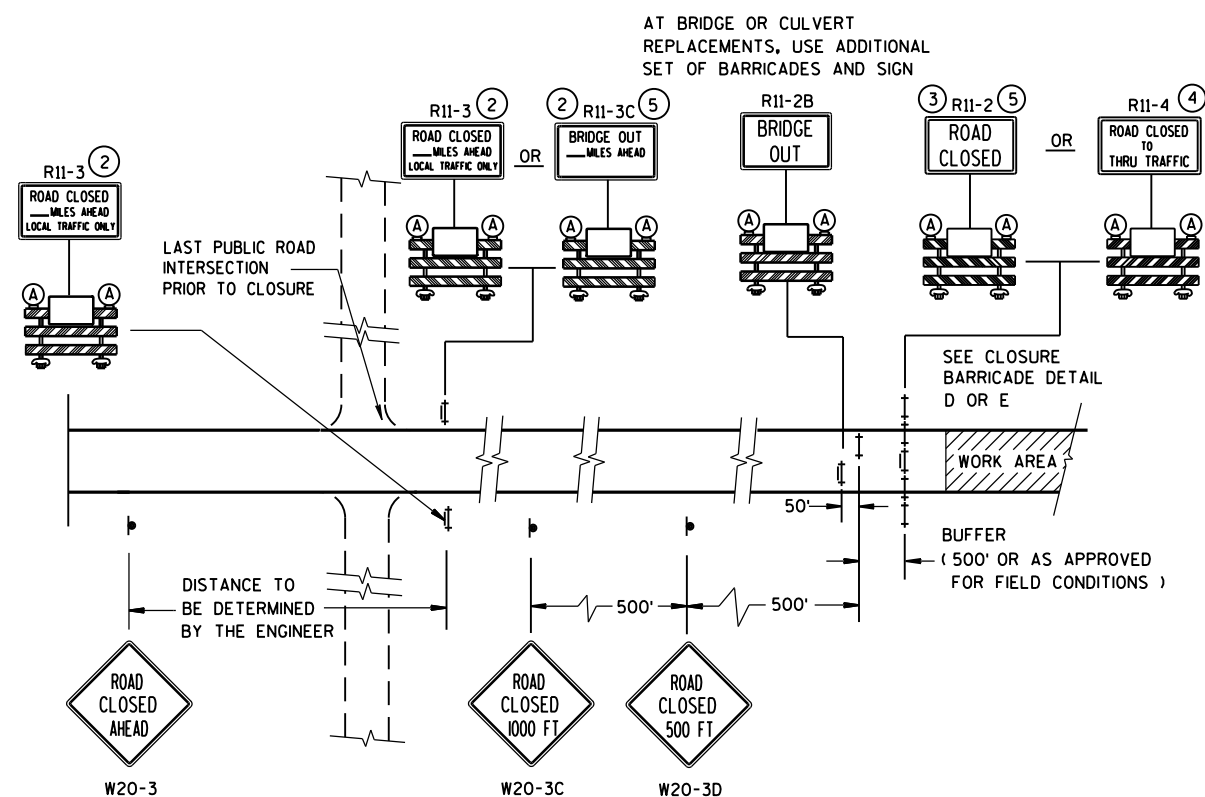
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR













WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C

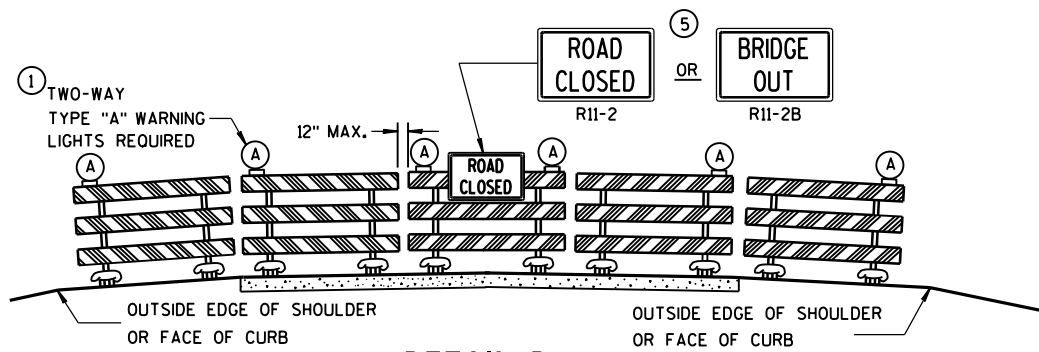
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (F T)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

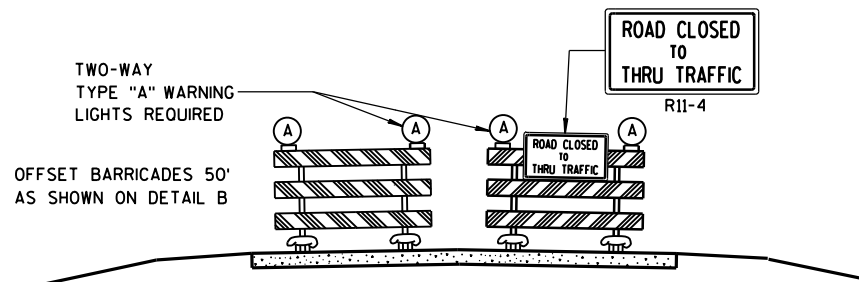
- # LEGEND
-  SIGN ON PERMANENT SUPPORT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  WORK AREA
-  M4-8
 M3-X
-  M1-4 OR  M1-5A OR  M1-6
-  M05-1 OR  M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

<p>BARRICADES AND SIGNS FOR MAINLINE CLOSURES</p>	
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>	
<p><u>Sept. 2015</u></p> <p><u>DATE</u></p>	<p><u>/S/ Peter Amakobe Atepe</u></p> <p><u>STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER</u></p>
<p>FHWA</p>	



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

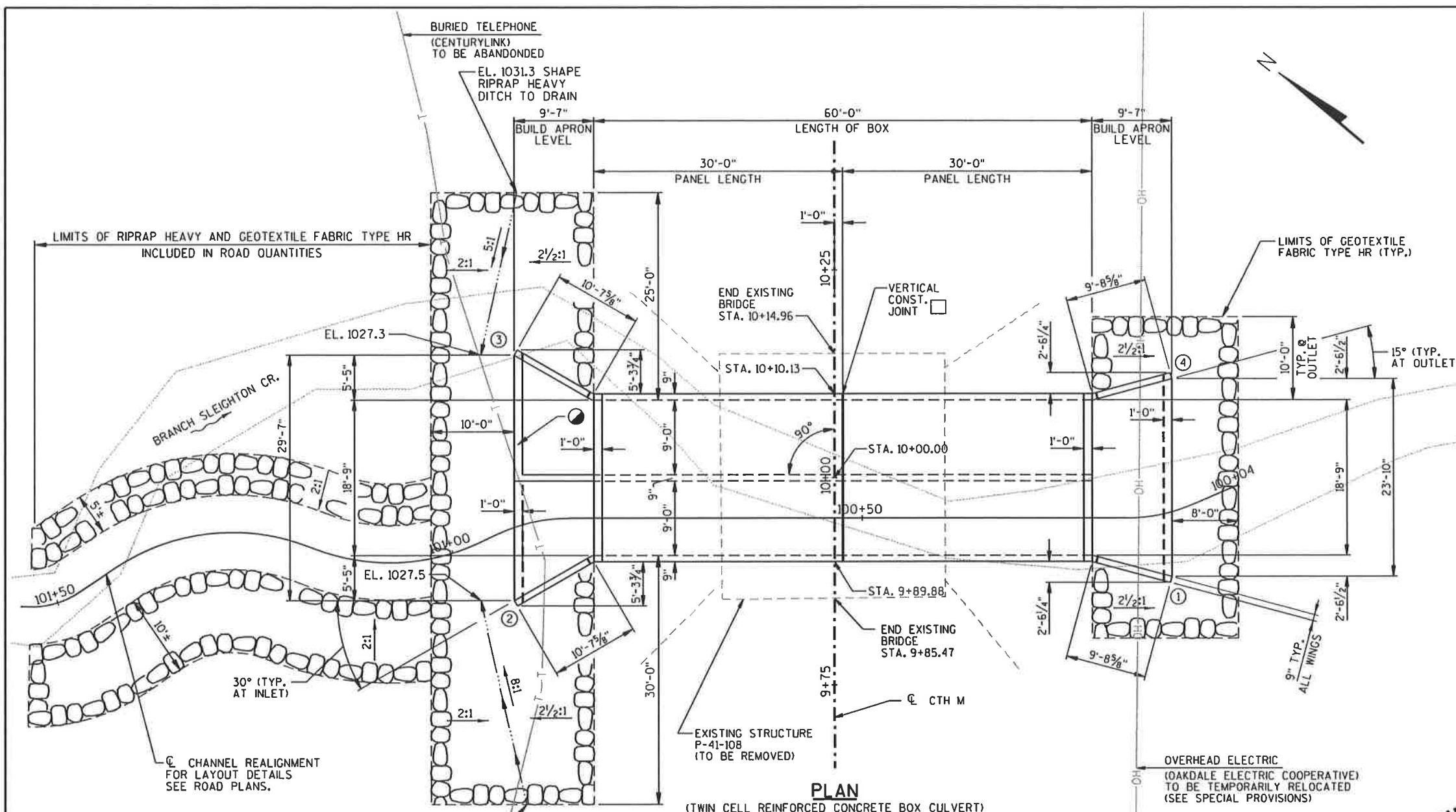
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

- 1 TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- 2 THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- 3 FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- 4 FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- 5 FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- 6 INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- 7 "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

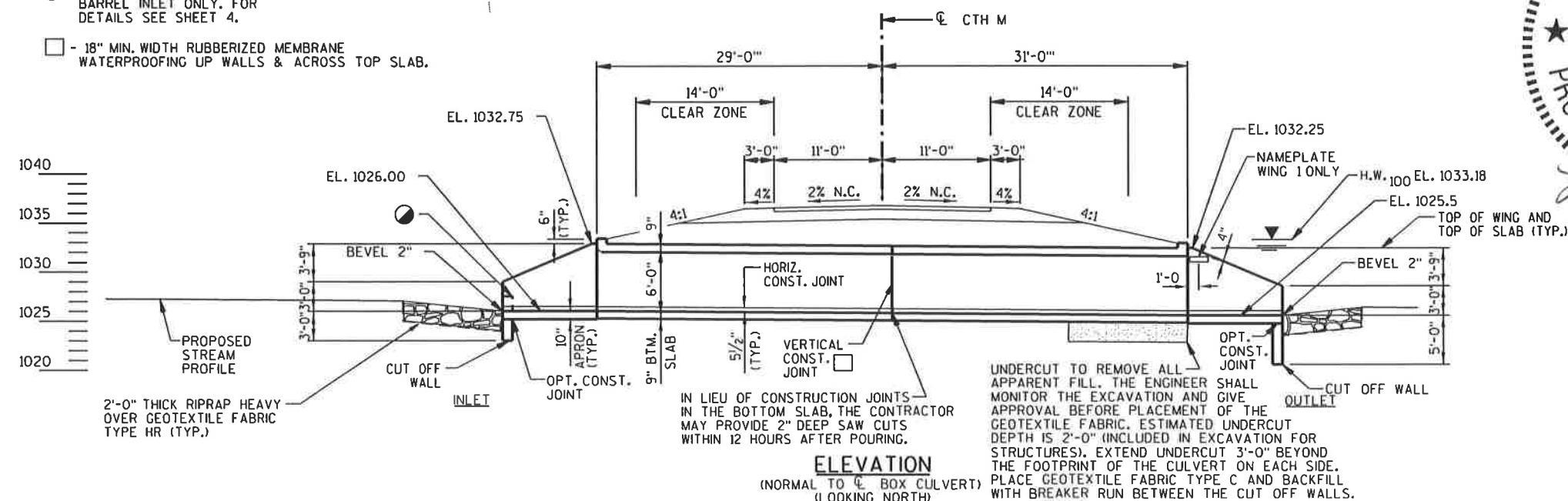
BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Sept. 2015 DATE	/S/ Peter Amokobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

BENCHMARKS			
NO.	STA./OFFSET	DESCRIPTION	ELEVATION
1	10+13.8, 13.2' RT	CHIS + ON NE CORNER BRIDGE WING	1035.3
2	10+03.0, 64.8' LT	2 POLE SPIKES IN CLUMP OF TREES	1031.7
3	10+54.8, 177.5' RT	2 POLE SPIKES IN 12" BOX ELDER	1031.2

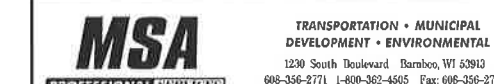
1. GENERAL PLAN
2. CROSS SECTION, QUANTITIES, NOTES & DETAILS
3. BOX CULVERT
4. INLET APRON & WING DETAILS
5. OUTLET APRON & WING DETAILS
6. SUBSURFACE EXPLORATION



- - INDICATES WING NUMBER
 - - WEIR EL. 1027.5, NORTH BARREL INLET ONLY. FOR DETAILS SEE SHEET 4.
 - - 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING UP WALLS & ACROSS TOP SLAB.
- EL. 1030.6 SHA
RIPRAP HEAVY
DITCH TO DRAIN



NO.	DATE	REVISION	BY
-----	------	----------	----

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED William C. Dreher SDR 05/06/1
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE	C-41-136
-----------	----------

CTH M OVER BRANCH SLEIGHTON CREEK			
COUNTY	MONROE	TOWN/CITY/VILLAGE	WIL T

DESIGN SPEC.		AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS	
DESIGNED BY	JAS	DESIGN CK'D	IRS
DRAWN BY	RJR	PLANS CK'D	

BT	JAS	CRD.	JRS	BT	RER	CRD.	ES
					SHEET 1 OF		

GENERAL PLAN

[illegible]

STRUCTURE ESTIMATED QUANTITIES

ITEM NUMBER	BID ITEM	UNIT	TOTAL
203.0500.S.01	REMOVING OLD STRUCTURE OVER WATERWAY STATION 10+00	LS	1
206.2000.01	EXCAVATION FOR STRUCTURES CULVERTS C-41-136	LS	1
210.0100	BACKFILL STRUCTURE	CY	775
311.0115	BREAKER RUN	CY	160
504.0100	CONCRETE MASONRY CULVERTS	CY	126
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	17,130
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	17
606.0300	RIPRAP HEAVY	CY	137
645.0105	GEOTEXTILE FABRIC TYPE C	SY	285
645.0120	GEOTEXTILE FABRIC TYPE HR	SY	284
SPV.0105.01	TEMPORARY WATER DIVERSION, CULVERT C-41-136	LS	1
	NON-BID ITEMS		
	PREFORMED FILLER	SIZE	¾"

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.

THE FIRST DIGIT OF A THREE DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

THE CONCRETE IN THE CUT OFF WALL MAY BE PLACED UNDERWATER IF THE EXCAVATION CANNOT BE DEWATERED.

THE ALTERNATE CUT OFF WALL MAY BE USED IN LIEU OF THE CAST IN PLACE CUT OFF WALLS.
PAYMENT SHALL BE BASED ON CONCRETE CUT OFF WALLS.

ALL SPACES EXCAVATED AND NOT OCCUPIED BY THE NEW STRUCTURE AND BREAKER RUN SHALL BE BACKFILLED WITH BACKFILL STRUCTURE TO THE ELEVATION OF THE ROADWAY SUBGRADE. BACKFILL STRUCTURE IS REQUIRED BEHIND ALL WINGWALLS. THE BACKFILL STRUCTURE ESTIMATED QUANTITIES ASSUMED A 1½:1 EXCAVATION SLOPE.

THIS STRUCTURE WILL REPLACE EXISTING BRIDGE P-41-108, A 29.3 FT. LONG SINGLE SPAN CONCRETE DECK GIRDER BRIDGE ON FULL RETAINING CONCRETE ABUTMENTS.

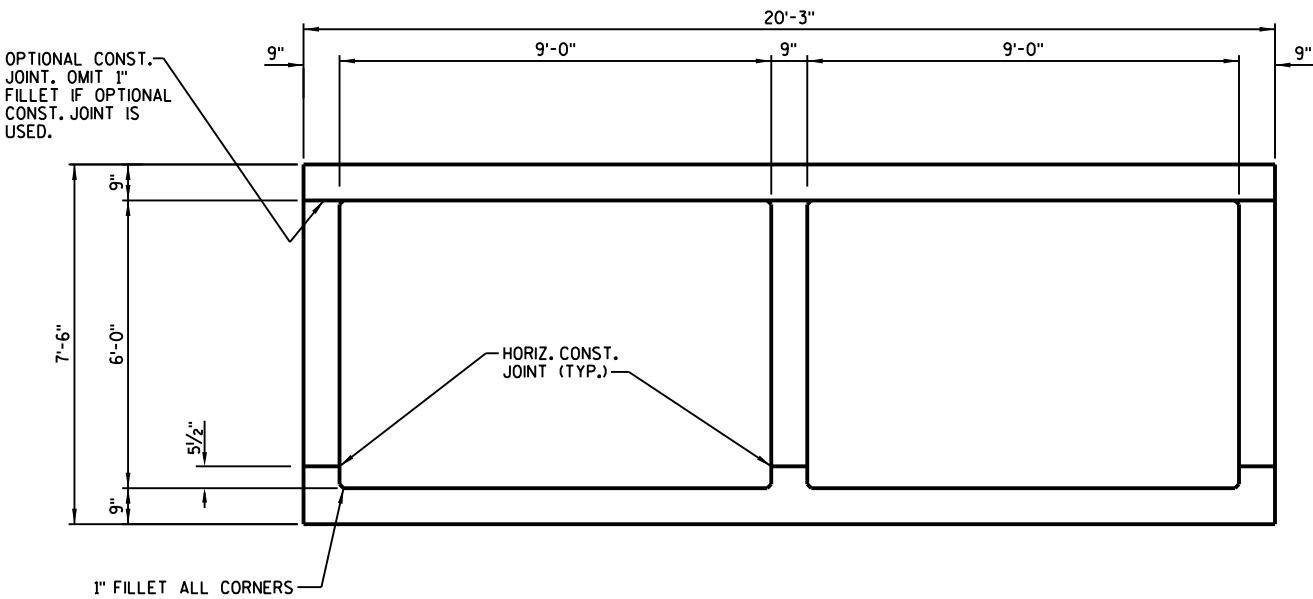
THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES CULVERTS C-41-136" SHALL BE THE EXISTING GROUND LINE.

THE CONTRACTOR MAY FURNISH A PRECAST CONCRETE BOX CULVERT IN LIEU OF THE CAST-IN-PLACE BOX CULVERT WITH THE ACCEPTANCE OF THE SHOP DRAWINGS BY THE STRUCTURES DESIGN SECTION. THE PRECAST CONCRETE BOX CULVERT SHALL CONFORM TO PRECAST DETAILS IN CHAPTER 36 STANDARDS OF THE CURRENT WISCONSIN DOT BRIDGE MANUAL. PAYMENT FOR THE PRECAST CULVERT SHALL BE BASED ON THE QUANTITIES AND PRICES BID FOR THE ITEMS LISTED IN THE "STRUCTURE ESTIMATED QUANTITIES".

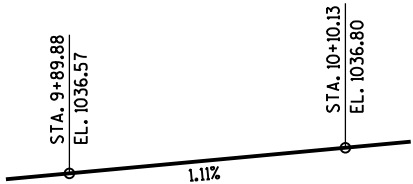
IN LIEU OF USING BREAKER RUN FOR THE BOX CONSTRUCTION PLATFORM, THE CONTRACTOR MAY ELECT TO SUBSTITUTE #1 OR #2 CONCRETE COARSE AGGREGATE, SELECT CRUSHED MATERIAL OR OTHER GRANULAR MATERIAL AS APPROVED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR BASE STABILITY WITH ANY SUBSTITUTED MATERIAL. THE REGION GEOTECHNICAL ENGINEER MAY BE CONTACTED TO DETERMINE IF "OTHER GRANULAR MATERIAL" IS ACCEPTABLE.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO NAVD 88 (1996 ADJUSTED), AND WERE ESTABLISHED AT THE SITE USING GPS TECHNOLOGY.

APRONS AND BOTTOM SLAB MAY BE POURED CONTINUOUSLY.



TYPICAL SECTION THRU BOX



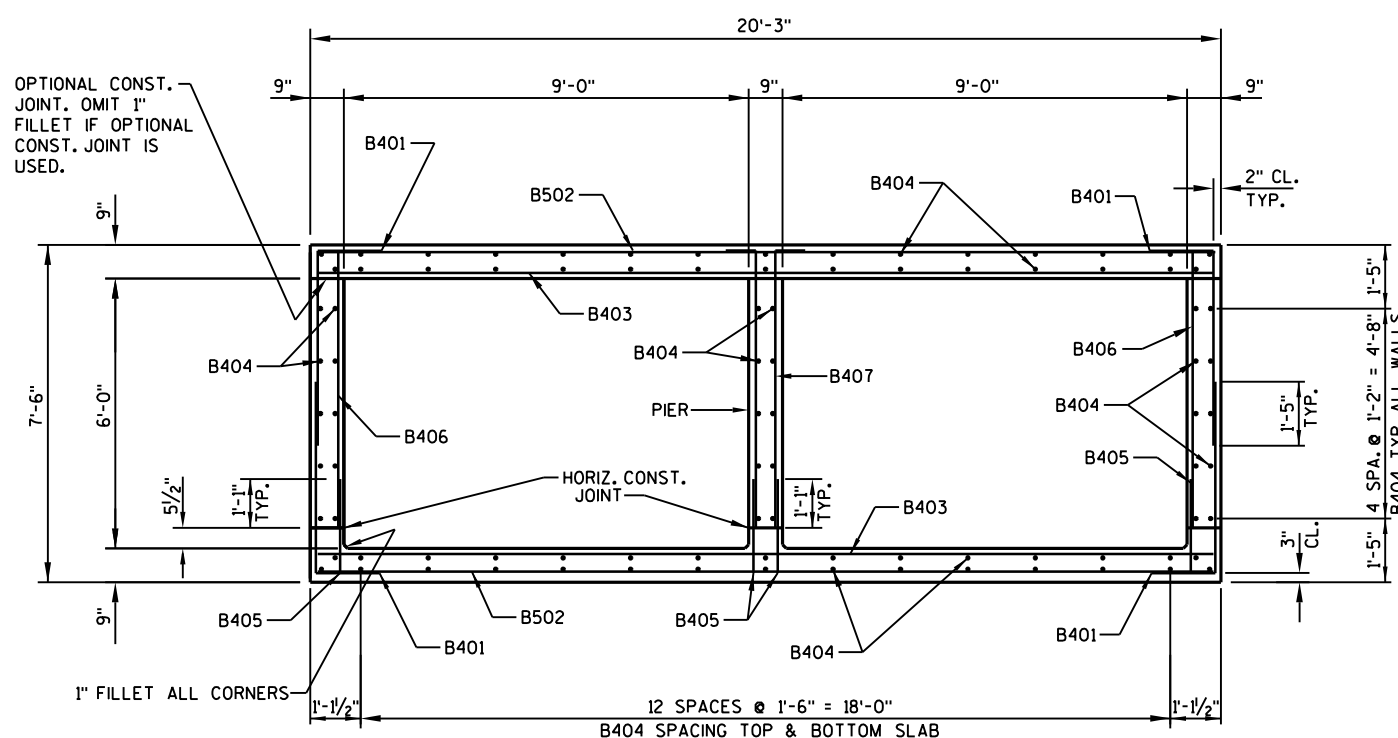
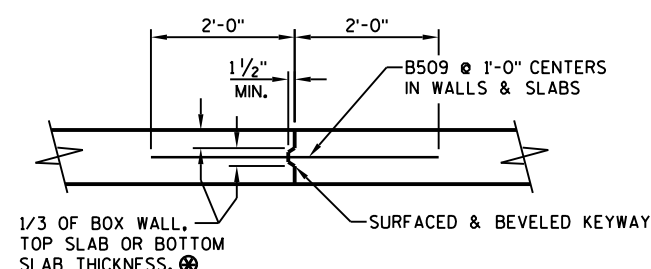
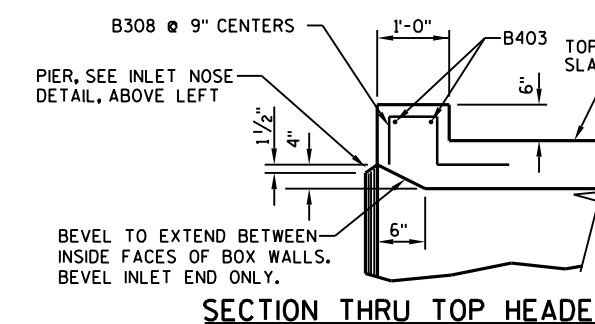
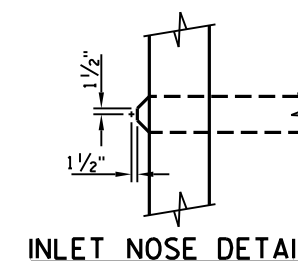
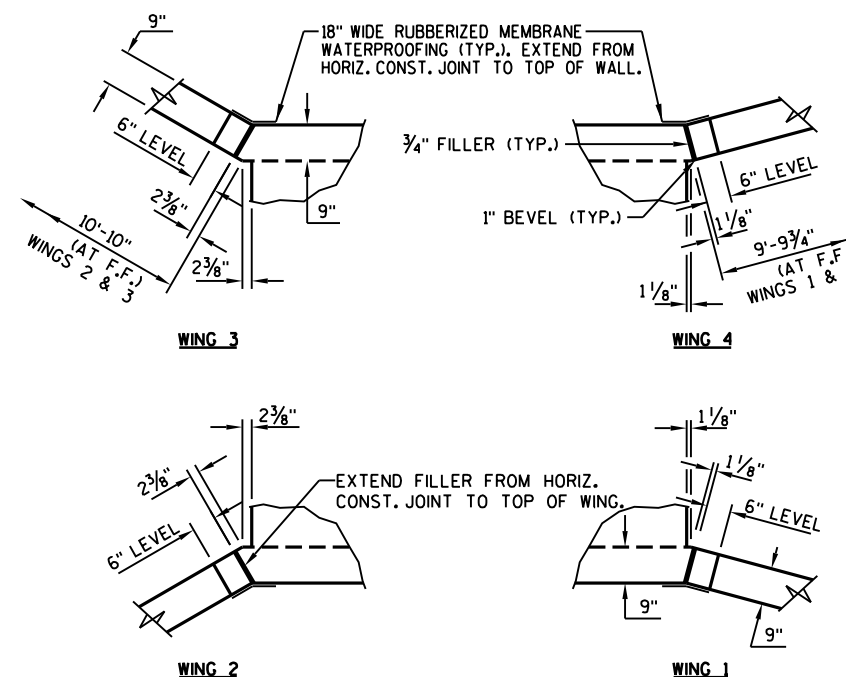
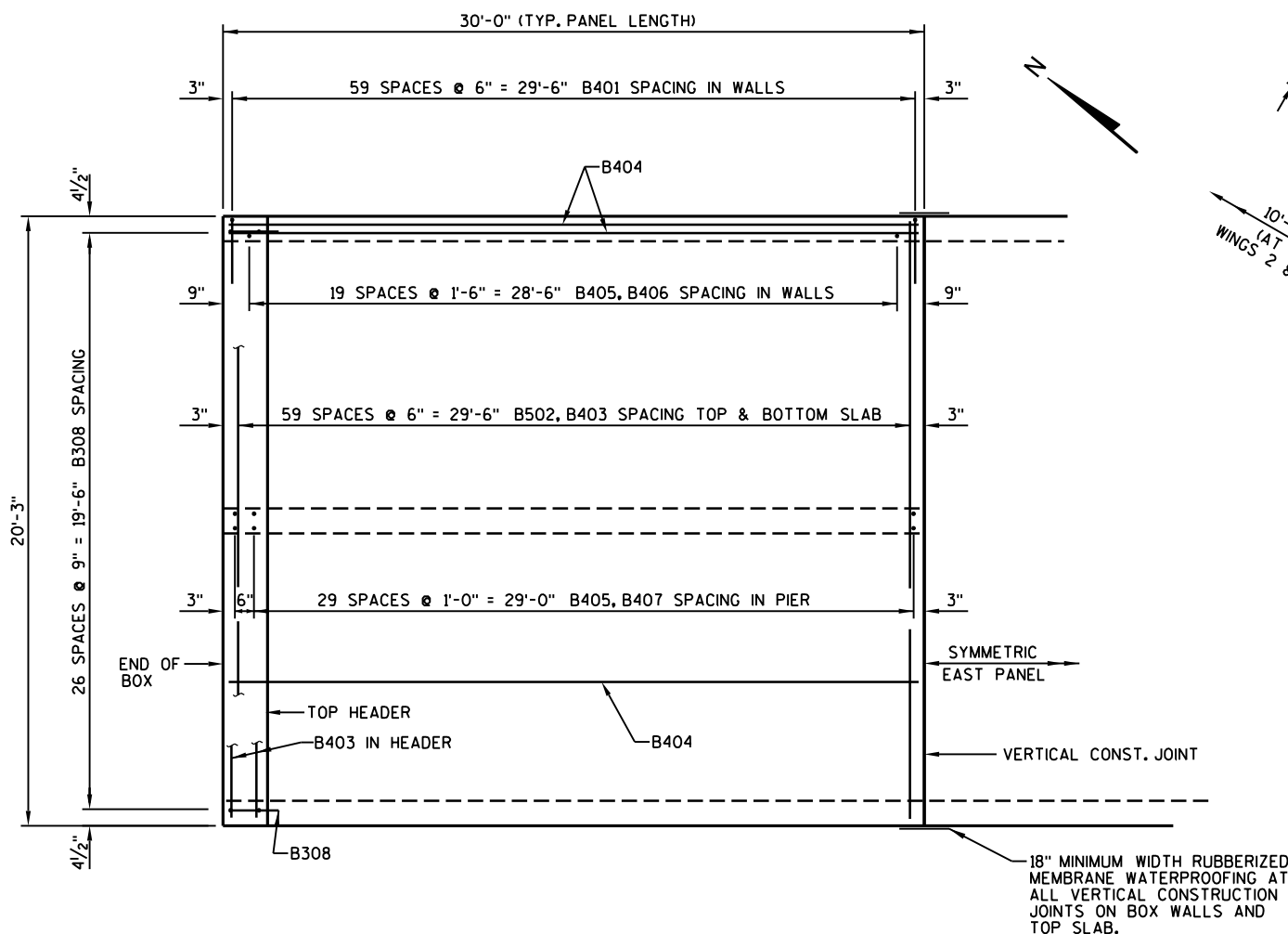
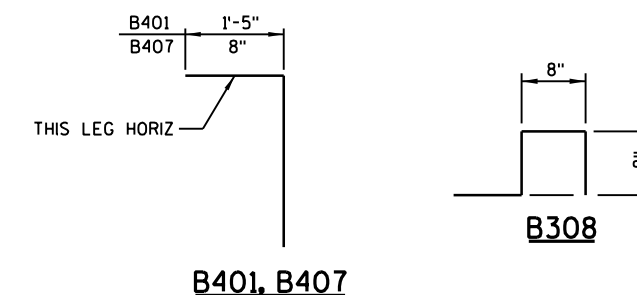
PROFILE GRADE LINE - CTH M

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE C-41-136			
DRAWN BY RLR		PLANS CK'D. LJR	
CROSS SECTION, QUANTITIES, NOTES & DETAILS			SHEET 2 OF 6

BILL OF BARS (UNCOATED) 15,045 LBS.

MARK	NO. REQ'D.	LENGTH	BENT	LOCATION
B401	480	5'-7"	X	BOX CORNERS - TOP & BOTTOM - TRANS.
B502	240	19'-11"		BOX EXTERIOR SLABS - TRANS.
B403	244	19'-11"		BOX INTERIOR SLABS & HEADERS - TRANS.
B404	180	29'-8"		BOX SLABS & WALLS - LONGIT.
B405	204	2'-1"		BOX WALL DOWELS - VERT.
B406	80	6'-1"		BOX INTERIOR WALLS - VERT.
B407	124	6'-8"	X	BOX PIER - VERT.
B308	54	2'-3"	X	BOX HEADER STIRRUPS - VERT.
B509	60	4'-0"		BOX VERT. CONST. JOINT DOWELS - VERT.

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.



ALL LONGITUDINAL BARS NOT IDENTIFIED ARE B404 AS SHOWN.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE C-41-136			
DRAWN BY RLR		PLANS CK'D. JAS	
BOX CULVERT		SHEET 3 OF 6	

UNCOATED 1125 LBS.

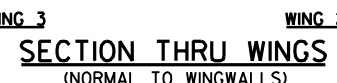
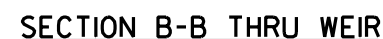
* - LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS. BENT BARS IF USED IN BAR SERIES TABLE SHALL BE BENT AFTER CUTTING.

MARK	NO. REQUIRED	LENGTH
A403	1 SERIES OF 9	20'-1" TO 29'-4"
A405	2 SERIES OF 4	3'-2" TO 8'-5"
A407	2 SERIES OF 4	7'-1" TO 8'-1"
A409	2 SERIES OF 14	3'-9" TO 6'-1"
A410	2 SERIES OF 8	2'-5" TO 6'-1"

MARK	A
A401	1'-0"
A407	3'-8"
A408	6'-1"
A415	8"
A417	1'-6"
A418	1'-10"

THIS LEG
HORIZ.

FILE= 5848016_04.dgn



○ — INDICATES WING NUMBER
F.F.— FRONT FACE
B.F.— BACK FACE
CL. — CLEAR



BILL OF BARS UNCOATED 960 LBS.

MARK	NUMBER REQUIRED	LENGTH	BENT	BAR SERIES	LOCATION
C401	24	5'-6"	X		OUTLET APRON CUT-OFF WALL - VERT.
C402	5	24'-10"			OUTLET APRON CUT-OFF WALL - TRANS.
C403	9	22'-2"		*	OUTLET APRON - TRANS.
C404	20	11'-8"			OUTLET APRON - LONGIT.
C405	2	7'-4"			OUTLET APRON @ WINGS - LONGIT.
C406	2	3'-7"			OUTLET APRON @ WINGS - LONGIT.
C407	14	2'-6"			OUTLET WINGS - F.F. DOWELS - VERT.
C408	2	7'-1"	X		OUTLET WINGS - B.F. - END - VERT.
C409	2	7'-6"	X		OUTLET WINGS - B.F. - END - VERT.
C410	2	7'-11"	X		OUTLET WINGS - B.F. - END - VERT.
C411	28	8'-7"	X		OUTLET WINGS - B.F. - VERT.
C412	28	4'-10"		*	OUTLET WINGS - B.F. - VERT.
C413	14	4'-3"		*	OUTLET WINGS - F.F. - VERT.
C414	12	9'-4"			OUTLET WINGS & APRON - LONGIT.
C415	4	6'-7"			OUTLET WINGS - LONGIT.
C416	4	2'-11"			OUTLET WINGS - LONGIT.
C517	4	10'-0"			OUTLET WINGS - TOP - LONGIT.

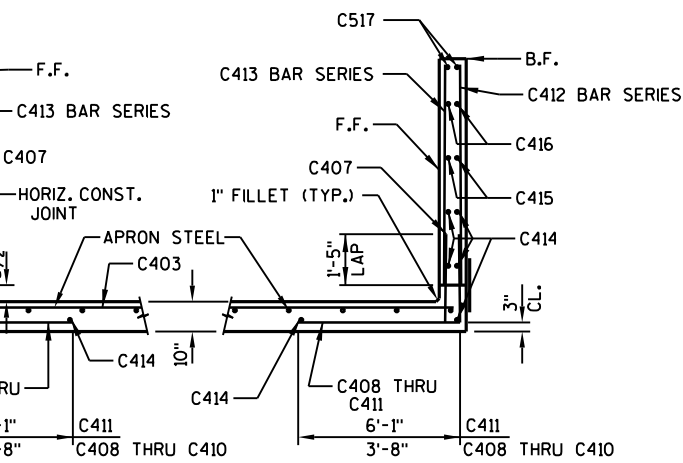
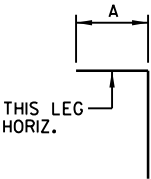
DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.
* - LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS. BENT BARS IF USED IN BAR SERIES TABLE SHALL BE BENT AFTER CUTTING.

BAR SERIES TABLE

MARK	NO. REQUIRED	LENGTH
C403	1 SERIES OF 9	20'-0" TO 24'-3"
C412	2 SERIES OF 14	3'-6" TO 6'-1"
C413	2 SERIES OF 7	2'-5" TO 6'-1"

BUNDLE AND TAG EACH SERIES SEPARATELY

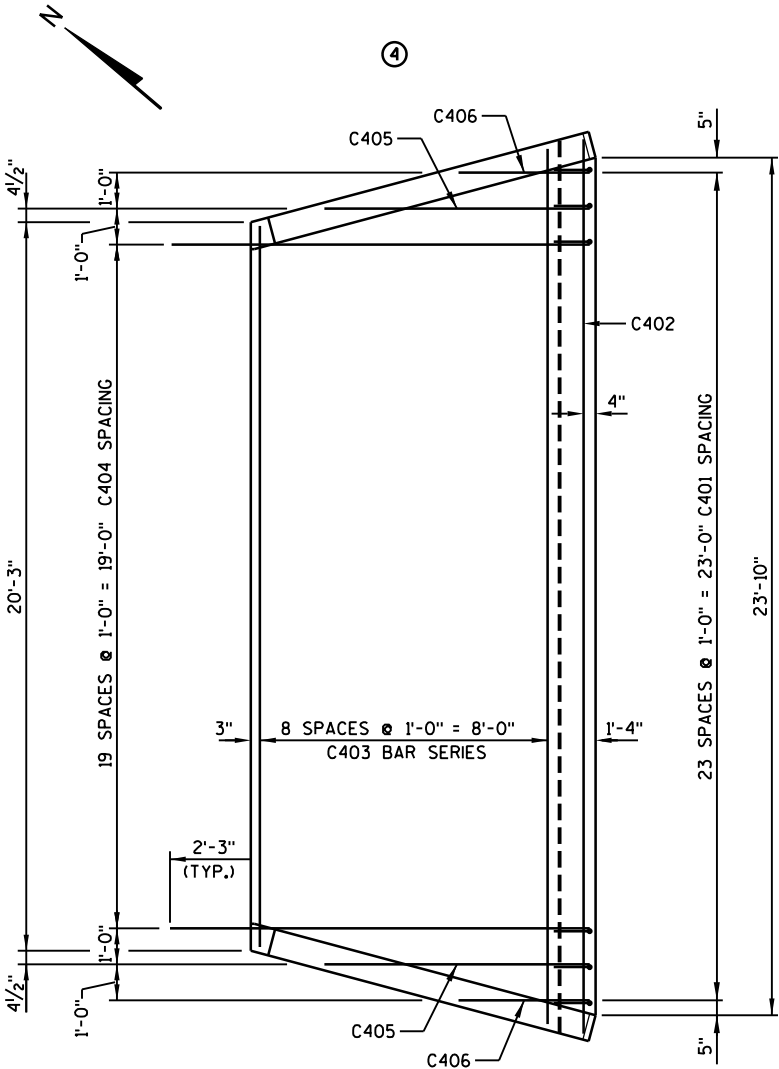
MARK	A
C401	1'-0"
C408	3'-8"
C409	3'-8"
C410	3'-8"
C411	6'-1"



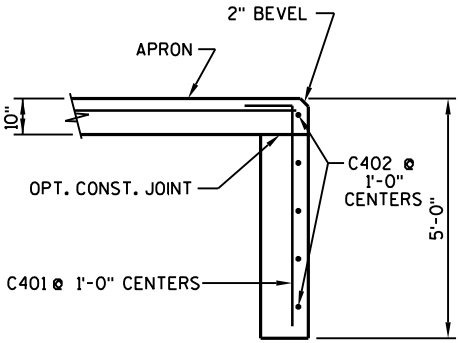
SECTION THRU WINGS (NORMAL TO WINGWALLS)

LEGEND

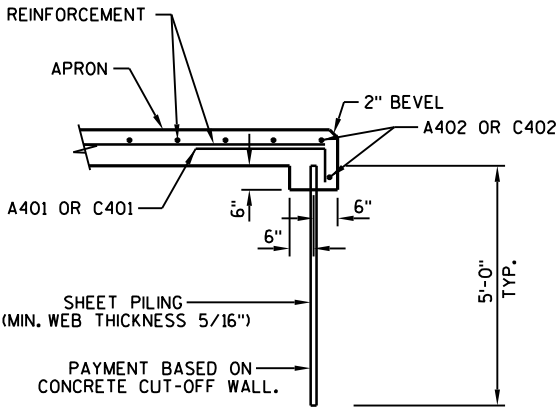
- — INDICATES WING NUMBER
- F.F. — FRONT FACE
- B.F. — BACK FACE
- CL. — CLEAR



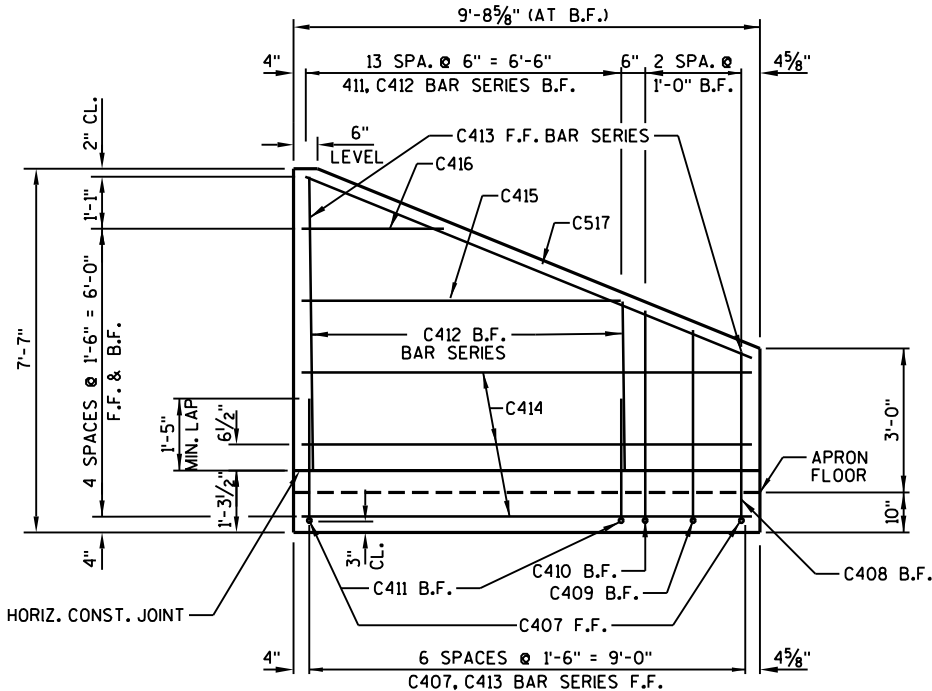
OUTLET APRON PLAN



SECTION THRU OUTLET CUT-OFF WALL

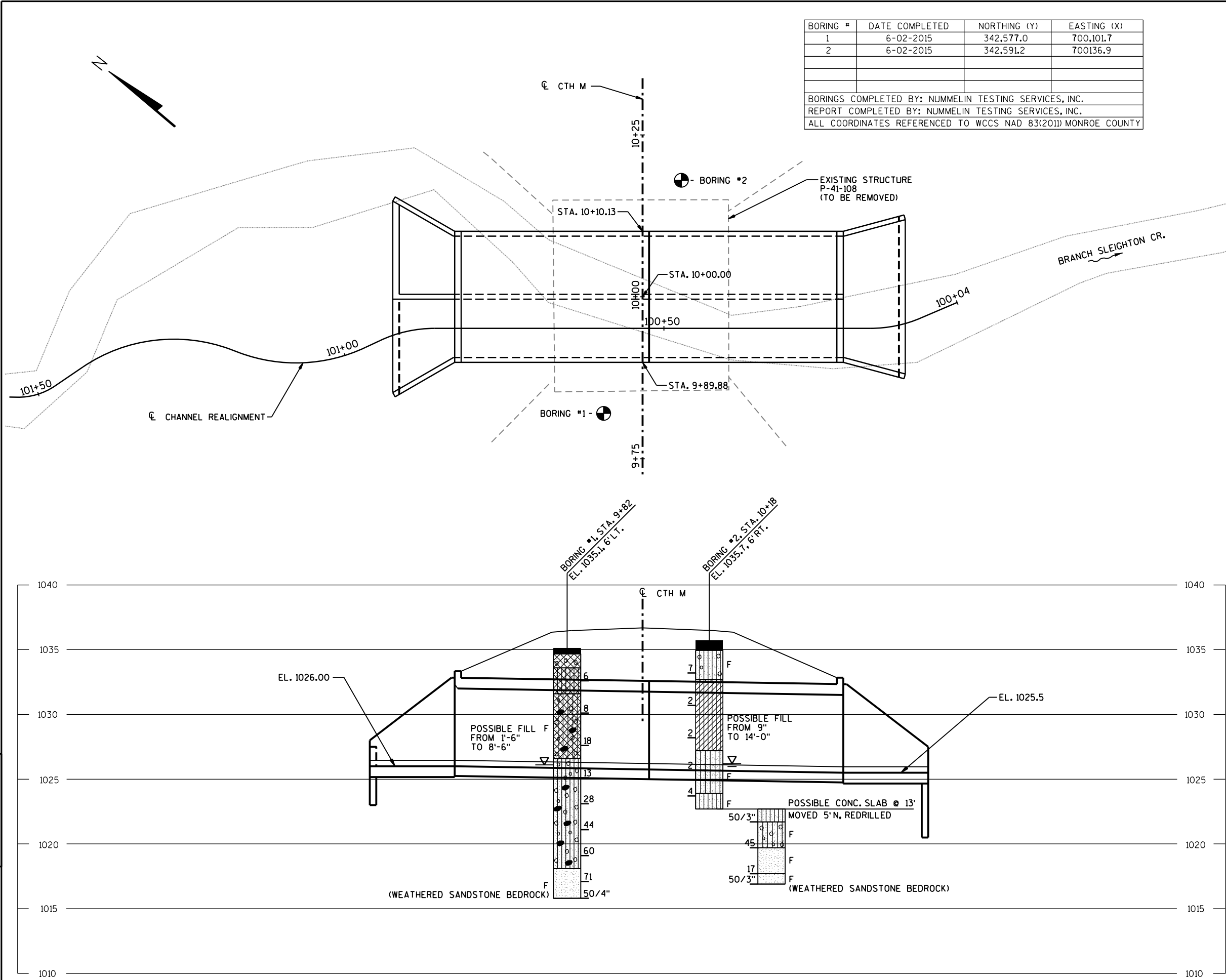


ALTERNATE CUT-OFF WALL



WING ELEVATION
OUTLET - WINGS 1 & 4

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE C-41-136			
DRAWN BY RLR		PLANS CK'D. JAS	
OUTLET APRON & WING DETAILS			SHEET 5 OF 6



BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	6-02-2015	342,577.0	700,101.7
2	6-02-2015	342,591.2	700136.9
BORINGS COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
REPORT COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
ALL COORDINATES REFERENCED TO WCCS NAD 83(2011) MONROE COUNTY			

STATE PROJECT NUMBER

5119-00-72

MATERIAL SYMBOLS

ASPHALT

CONCRETE

SAND

BOULDERS OR COBBLES

SHALE

TOPSOIL

FILL

CLAY

LIMESTONE

SANDSTONE

PEAT

GRAVEL

SILT

BEDROCK (UNKNOWN)

IGNEOUS/META

LEGEND OF BORING

BORING # EL. STA./OFF-SET

ST (1) 17

0.25

F-C

COBBLE OR BOULDER

WEATHERED LIMESTONE

CORE RUN #1 - 24'-29'

REC=80%, ROD=72%

GROUND WATER ELEVATION

AT TIME OF DRILLING

END OF DRILLING

AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

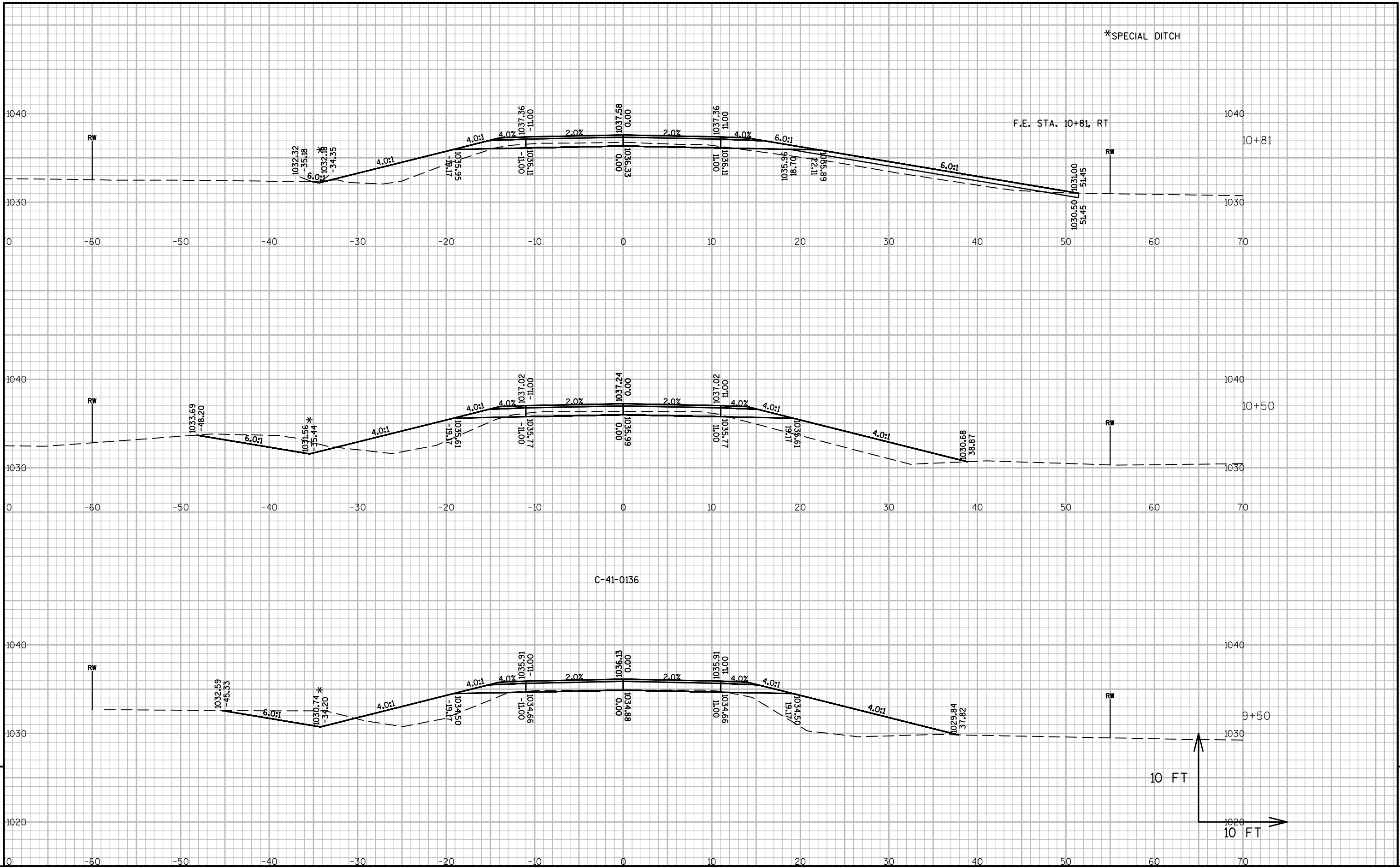
SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

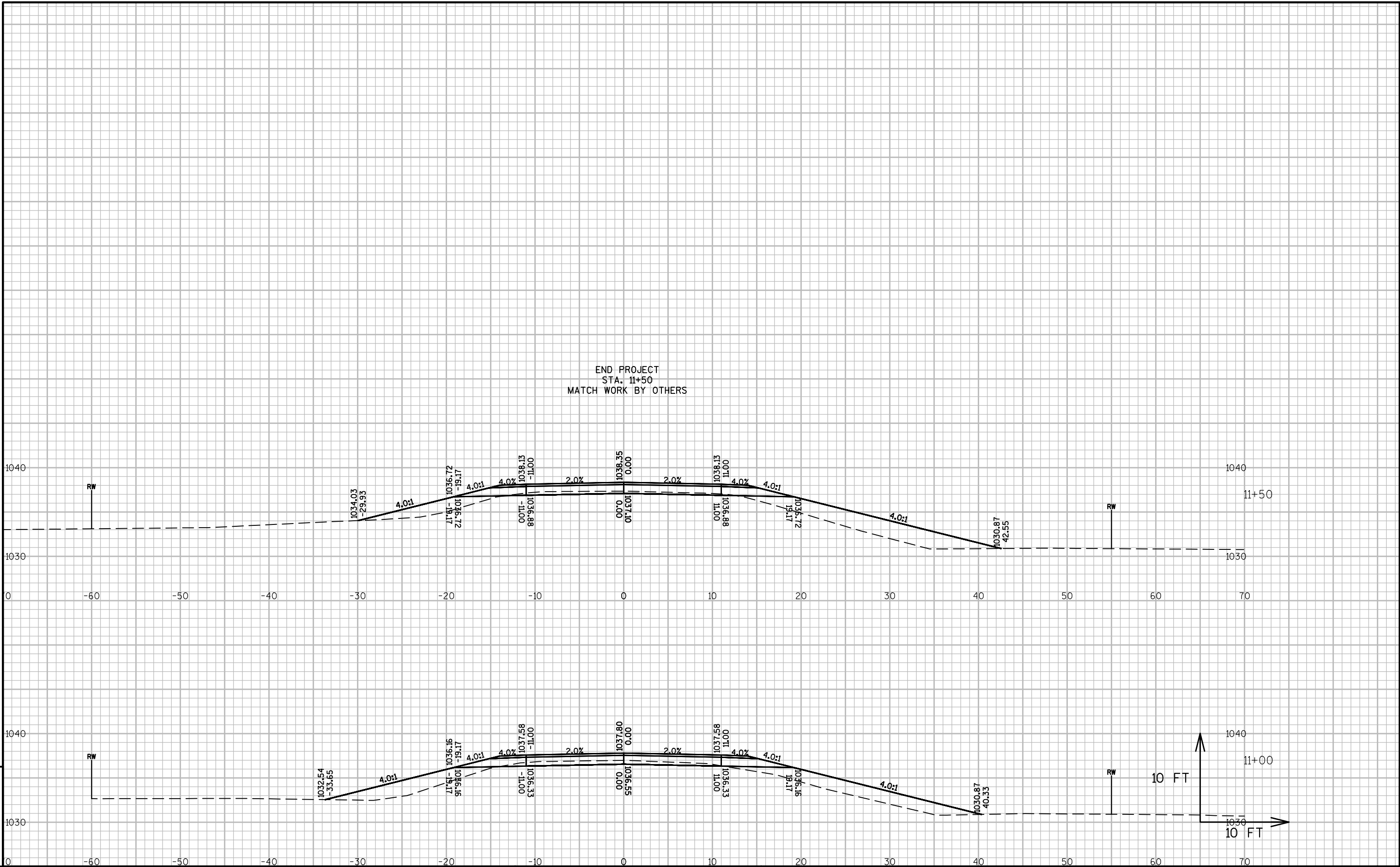
BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE C-41-136			
DRAWN BY		RLR	PLANS CK'D. JAS
SUBSURFACE EXPLORATION		SHEET 6 OF 6	

PROJECT I.D. 5119-00-72 EARTHWORK SUMMARY

STA	EXCAVATION COMMON CY	EXCAVATION ROCK CY	FILL (1) CY	EXPANDED FILL (2) CY	WASTE CY	BORROW CY
8+75.00	7	0	54	70	-63	63
9+00.00	21	0	133	173	-152	152
9+50.00	12	0	65	85	-73	73
9+72.00	STRUCTURE C-41-0136					
10+28.00	14	0	55	72	-58	58
10+50.00	11	0	63	82	-71	71
10+81.00	2	0	34	44	-42	42
11+00.00	3	0	102	133	-130	130
11+50.00						
SUBTOTALS						
WEST APPROACH	40	0	252	328	-288	288
EAST APPROACH	30	0	254	331	-301	301
TOTALS	70	0	506	659	-589	589
(1) - NOT A BID ITEM - FOR INFORMATIONAL PURPOSES ONLY. (2) - FILL EXPANSION 30%						







Wisconsin Department of Transportation

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SWL

NOV 2016

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile (Includes Erosion Control)
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 36

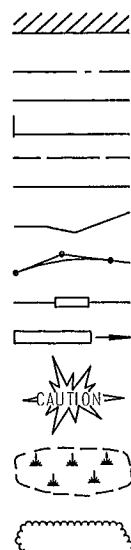


DESIGN DESIGNATION

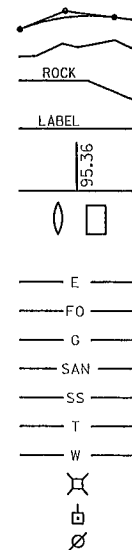
A.A.D.T.	2017	=	200
A.A.D.T.	2037	=	240
D.H.V.		=	40
D.D.		=	60/40
T.		=	7.0%
DESIGN SPEED		=	55 MPH
ESALS		=	29,200

CONVENTIONAL SYMBOLS

PLAN
CORPORATE LIMITS
PROPERTY LINE
LOT LINE
LIMITED HIGHWAY EASEMENT
EXISTING RIGHT OF WAY
PROPOSED OR NEW R/W LINE
SLOPE INTERCEPT
REFERENCE LINE
EXISTING CULVERT
PROPOSED CULVERT (Box or Pipe)
COMBUSTIBLE FLUIDS
MARSH AREA
WOODED OR SHRUB AREA



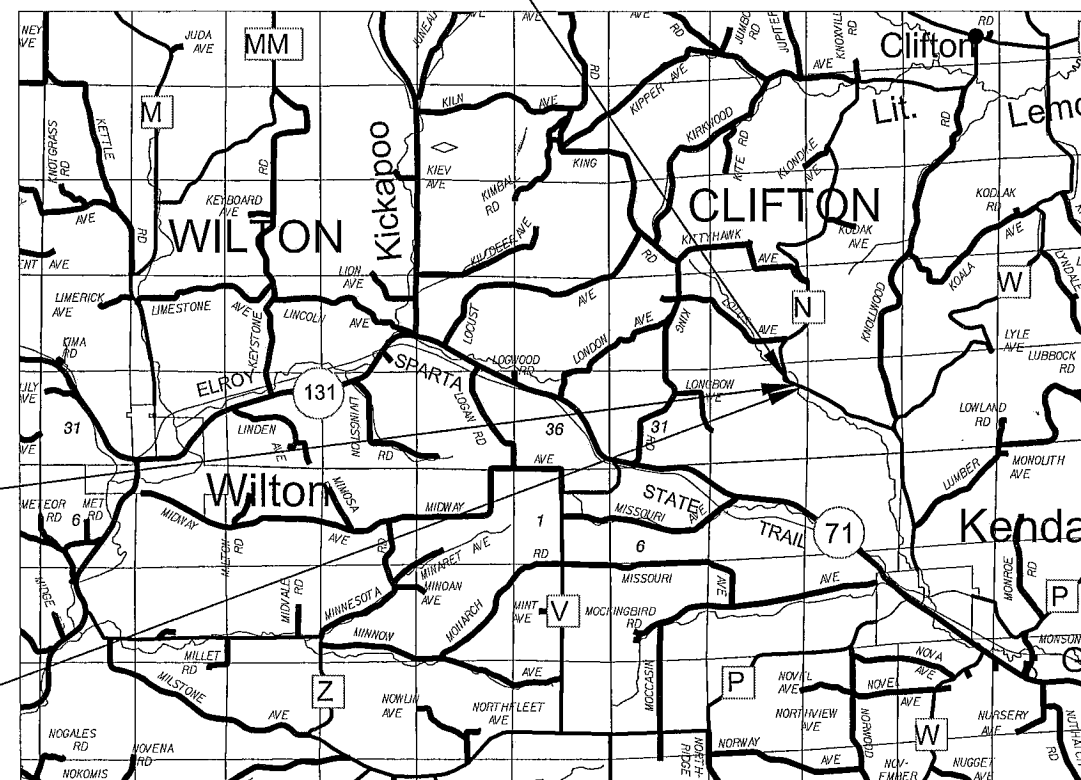
PROFILE
GRADE LINE
ORIGINAL GROUND
MARSH OR ROCK PROFILE (To be noted as such)
SPECIAL DITCH
GRADE ELEVATION
CULVERT (Profile View)
UTILITIES
ELECTRIC
FIBER OPTIC
GAS
SANITARY SEWER
STORM SEWER
TELEPHONE
WATER
UTILITY PEDESTAL
POWER POLE
TELEPHONE POLE



STRUCTURE B-41-0304

BEGIN PROJECT
STA. 9+00
Y = 336,552.53
X = 736,282.26

END PROJECT
STA. 11+00



R-1-W

R-1-E

LAYOUT

SCALE 0 2 MILES

TOTAL NET LENGTH OF CENTERLINE = 0.038

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
GLENDALE - CLIFTON
(SPRING VALLEY CREEK BRIDGE B-41-0304)
CTH N
MONROE COUNTY

STATE PROJECT NUMBER

5126-00-71

STATE PROJECT

5126-00-71

FEDERAL PROJECT

PROJECT

WISC 2016342

CONTRACT

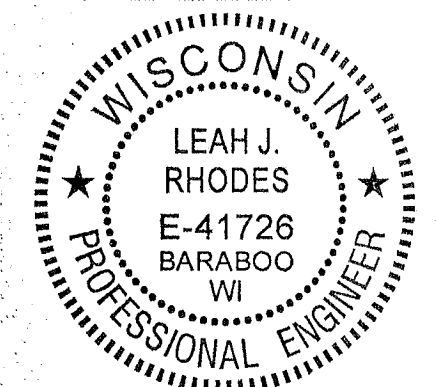
1

ACCEPTED FOR
COUNTY OF MONROE

04/08/2016 *[Signature]*
(Date) (Highway Commissioner)

ORIGINAL PLANS PREPARED BY

MSA
PROFESSIONAL SERVICES
TRANSPORTATION • MUNICIPAL
DEVELOPMENT • ENVIRONMENTAL
1230 South Boulevard • Baraboo, WI 53913
608-556-2771 • 1-800-562-4555 Fax: 608-556-2770



DATE: 4/4/2016 *[Signature]*
(Professional Engineer)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	MSA PROFESSIONAL SERVICES, INC.
Designer	MSA PROFESSIONAL SERVICES, INC.
Management Consultant	KL ENGINEERING, INC.

APPROVED FOR THE DEPARTMENT

DATE: 4/27/16 *[Signature]*
(Manager/Consultant Signature)

STANDARD ABBREVIATIONS

AC	ACRE	F/L	FLOW LINE	SALV	SALVAGED
AGG	AGGREGATE	FT	FOOT	SAN	SANITARY SEWER
<	ANGLE	GN	GRID NORTH	SECT	SECTION
ASPH	ASPHALTIC	HR	HANDICAP RAMP	SHLDR	SHOULDER
AC	ASPHALT CEMENT	HT	HEIGHT	SW	SIDEWALK
ADT	AVERAGE DAILY TRAFFIC	CWT	HUNDREDWEIGHT	S	SOUTH
B & B	BALLED AND BURLAPPED	HYD	HYDRANT	SB	SOUTHBOUND
BM	BENCH MARK	IN DIA	INCH DIAMETER	SPECS	SPECIFICATIONS
CB	CATCH BASIN	INL	INLET	SQ	SQUARE
℄ OR C/L	CENTER LINE	ID	INSIDE DIAMETER	SF OR SQ FT	SQUARE FEET
C-C	CENTER TO CENTER	I	INTERSECTION ANGLE	SY	SQUARE YARD
CONC	CONCRETE	IE	INVERT ELEVATION	SSPRC	STORM SEWER
CO	COUNTY	IP	IRON PIPE OR PIN		PIPE REINFORCED CONCRETE
CTH	COUNTY TRUNK HIGHWAY	JCT	JUNCTION	STD	STANDARD
CY	CUBIC YARD	L	LENGTH OF CURVE	SDD	STANDARD DETAIL DRAWINGS
CULV	CULVERT	LF	LINEAR FOOT	STH	STATE TRUNK HIGHWAYS
CP	CULVERT PIPE	LC	LONG CHORD OF CURVE	STA	STATION
CPRC	CULVERT PIPE	LCB	LONG CHORD BEARING	SS	STORM SEWER
	REINFORCED CONCRETE	LS	LUMP SUM	T	TANGENT
C & G	CURB AND GUTTER	MH	MANHOLE	TEL	TELEPHONE
D	DEGREE OF CURVE	N	NORTH	TEMP	TEMPORARY
DHV	DESIGN HOUR VOLUME	Y	NORTH GRID COORDINATE	TLE	TEMPORARY LIMITED EASEMENT
DIA OR ϕ	DIAMETER	OE	OUTLET ELEVATION	T	TON
DIST	DISTRICT	OL	OUT LOT	TC	TOP OF CURB
DWY	DRIVEWAY	OD	OUTSIDE DIAMETER	TN	TOWN
E	EAST	OH	OVERHEAD LINES	TRANS	TRANSITION
X	EAST GRID COORDINATE	PAVT	PAVEMENT	T	TRUCKS (percent of)
EB	EASTBOUND	PLE	PERMANENT LIMITED EASEMENT	TYP	TYPICAL
ELEC	ELECTRIC	PC	POINT OF CURVATURE	UNCL	UNCLASSIFIED
EL OR ELEV	ELEVATION	PI	POINT OF INTERSECTION	USH	UNITED STATES HIGHWAY
EMB	EMBANKMENT	PT	POINT OF TANGENCY	VAR	VARIABLE
EW	ENDWALL	PCC	PORTLAND CEMENT CONCRETE	VERT	VERTICAL
ESALS	EQUIVALENT SINGLE	LB	POUND	VC	VERTICAL CURVE
	AXLE LOADS	PE	PRIVATE ENTRANCE	VOL	VOLUME
EXC	EXCAVATION	R OR RAD	RADIUS	WM	WATER MAIN
EBS	EXCAVATION BELOW	RR	RAILROAD	WV	WATER VALVE
	SUBGRADE	R	RANGE	W	WEST
EXIST	EXISTING	℄ OR R/L	REFERENCE LINE	WB	WESTBOUND
EXP	EXPANSION	REQD	REQUIRED	YD	YARD
F-F	FACE TO FACE	RT	RIGHT		
FERT	FERTILIZER	R/W	RIGHT-OF-WAY		
FE	FIELD ENTRANCE	RD	ROAD		

DESIGN CONTACT

MSA PROFESSIONAL SERVICES, INC.
ATTN: LEAH RHODES, PE
1230 SOUTH BOULEVARD
BARABOO, WI 53913
608-355-8945
LRHODES@MSA-PS.COM

COUNTY CONTACT

MONROE COUNTY
ATTN: JACK DITTMAR, COMMISSIONER
803 WASHINGTON STREET
SPARTA, WI 54656
608-269-8740
JACK.DITTMAR@CO.MONROE.WI.US

DNR LIAISON

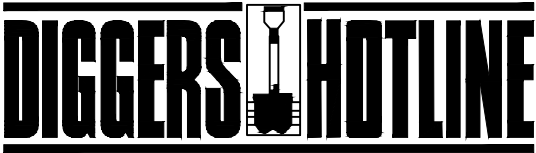
WISCONSIN DEPARTMENT OF
NATURAL RESOURCES
ATTN: KAREN KALVELAGE
ENVIRONMENTAL ANALYSIS AND
REVIEW SPECIALIST
3550 MORMON COULEE ROAD
LA CROSSE, WI 54601
608-785-9115
KAREN.KALVELAGE@WISCONSIN.GOV

UTILITIES

COMMUNICATION:
CENTURYLINK
ATTN: BRET CLARK
311 SOUTH COURT STREET
SPARTA, WI 54656
608-269-0819
BRET.CLARK@CENTURYLINK.COM

ELECTRIC:
OAKDALE ELECTRIC COOPERATIVE
ATTN: TRAVIS CHAMPLIN
489 N OAKWOOD STREET
P.O. BOX 128
OAKDALE, WI 54649
608-372-8848
TRAVISC@OAKDALEREC.COM

* NOT A MEMBER OF
DIGGERS HOTLINE



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www.DiggersHotline.com

GENERAL NOTES

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE FERTILIZED, SEEDED AND MULCHED AS DIRECTED BY THE ENGINEER. OVERSOW PERMANENT SEEDING AREAS WITH TEMPORARY SEED AT 3 LBS. PER 1000 SQUARE FEET.

THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE AREA THAT ARE NOT SHOWN.

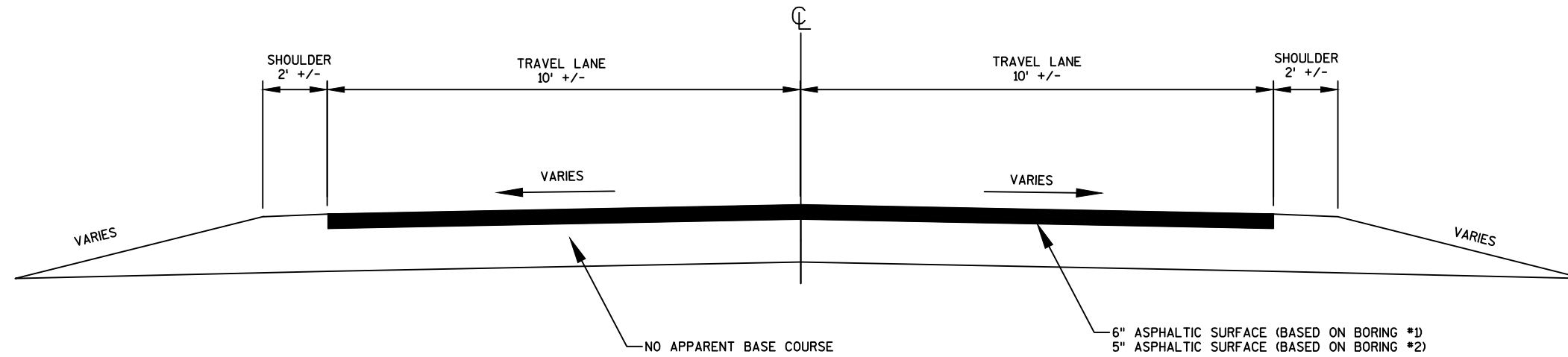
NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO USGS NAVD 88 (1996 ADJUSTED). BENCHMARKS WERE LOCATED IN THE FIELD USING GPS TECHNOLOGY.

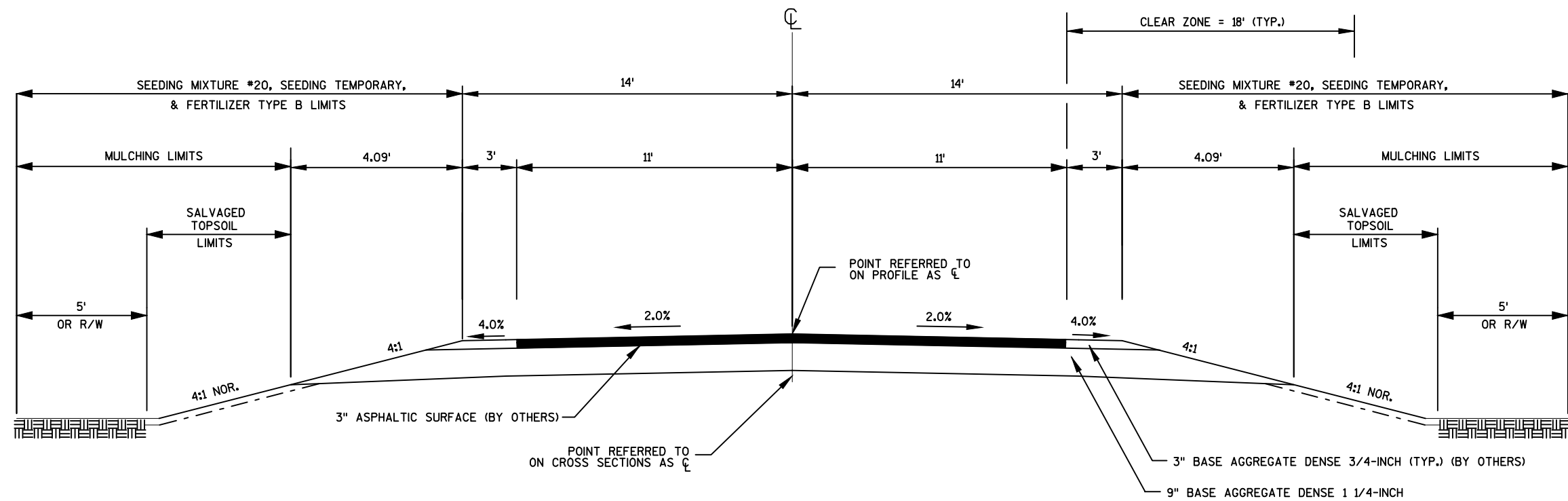
SILT FENCE TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER AND IN PLACE PRIOR TO CONSTRUCTION OR BRIDGE REMOVAL.

WETLANDS ARE PRESENT OUTSIDE THE EXISTING TOE OF SLOPE. AREAS OUTSIDE THE SLOPE INTERCEPTS SHALL NOT BE DISTURBED.

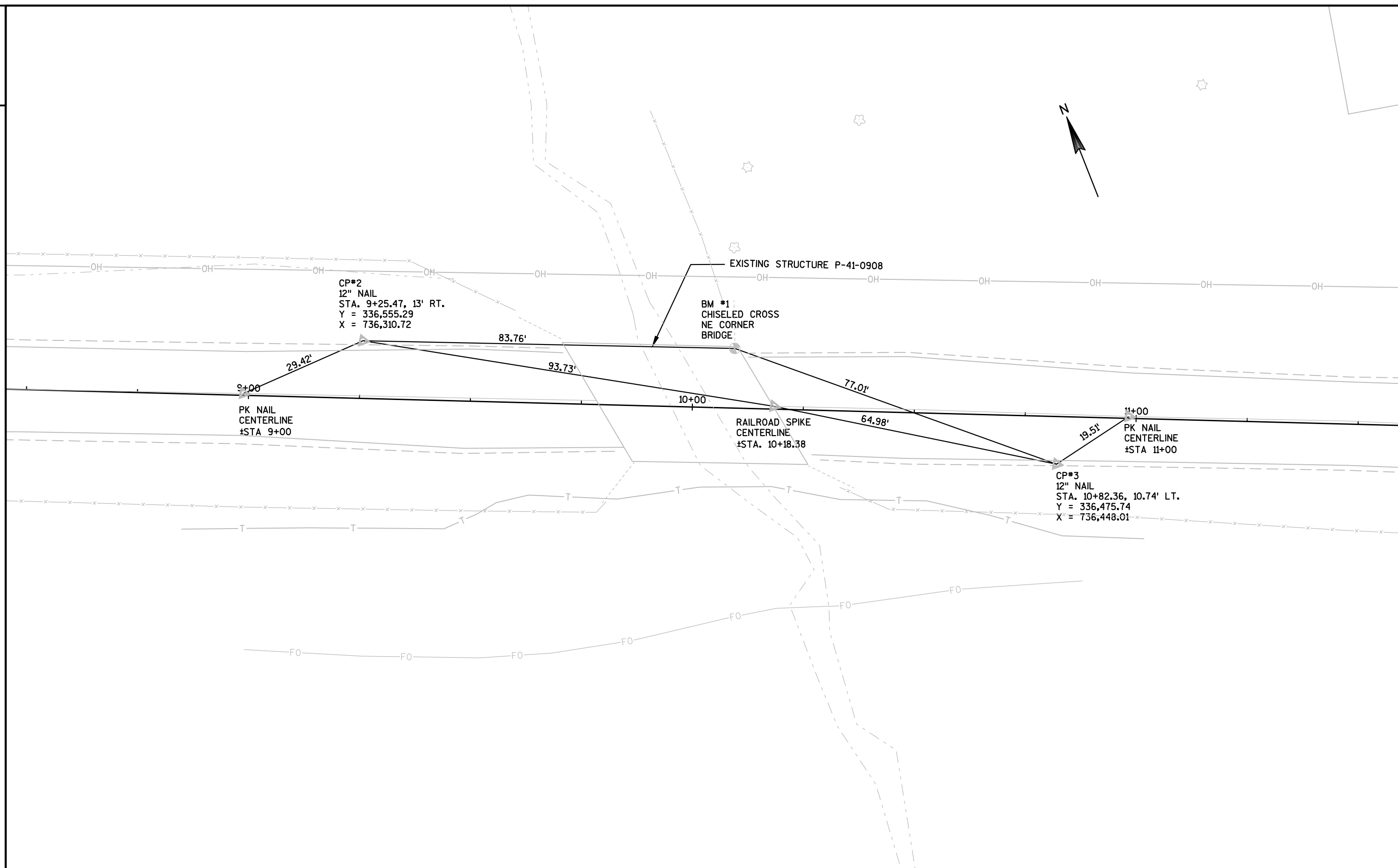
TOTAL PROJECT AREA = 0.48 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.34 ACRES



EXISTING TYPICAL SECTION
CTH N



FINISHED TYPICAL SECTION
CTH N



DATE 16AUG16		E S T I M A T E O F Q U A N T I T I E S			
LINE					5126-00-71
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0030	203.0600.S	Removing Old Structure Over Waterway With Minimal Debris (station) 01. 10+00	LS	1.000	1.000
0040	205.0100	Excavation Common **P**	CY	127.000	127.000
0060	206.2000	Excavation for Structures Culverts (structure) 02. B-41-304	LS	1.000	1.000
0070	208.0100	Borrow	CY	188.000	188.000
0080	210.0100	Backfill Structure	CY	1,055.000	1,055.000
0100	213.0100	Finishing Roadway (project) 02. 5126-00-71	EACH	1.000	1.000
0110	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	345.000	345.000
0120	311.0115	Breaker Run	CY	125.000	125.000
0130	504.0100	Concrete Masonry Culverts	CY	223.000	223.000
0140	505.0400	Bar Steel Reinforcement HS Structures	LB	32,170.000	32,170.000
0150	516.0500	Rubberized Membrane Waterproofing	SY	30.000	30.000
0160	606.0300	Riprap Heavy	CY	110.000	110.000
0180	619.1000	Mobilization	EACH	0.566	0.566
0190	624.0100	Water	MGAL	32.000	32.000
0200	625.0500	Salvaged Topsoil	SY	625.000	625.000
0210	627.0200	Mulching	SY	920.000	920.000
0220	628.1504	Silt Fence	LF	500.000	500.000
0230	628.1520	Silt Fence Maintenance	LF	500.000	500.000
0240	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0250	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0270	629.0210	Fertilizer Type B	CWT	0.650	0.650
0280	630.0120	Seeding Mixture No. 20	LB	30.000	30.000
0290	630.0200	Seeding Temporary	LB	30.000	30.000
0300	633.5100	Markers Row	EACH	8.000	8.000
0310	633.5200	Markers Culvert End	EACH	4.000	4.000
0320	638.2602	Removing Signs Type II	EACH	4.000	4.000
0330	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0340	642.5001	Field Office Type B	EACH	0.560	0.560
0360	643.0100	Traffic Control (project) 02. 5126-00-71	EACH	1.000	1.000
0370	645.0105	Geotextile Type C	SY	400.000	400.000
0380	645.0120	Geotextile Type HR	SY	250.000	250.000
0390	650.4500	Construction Staking Subgrade	LF	200.000	200.000
0400	650.5000	Construction Staking Base	LF	200.000	200.000
0420	650.6500	Construction Staking Structure Layout (structure) 02. B-41-304	LS	1.000	1.000
0440	650.9910	Construction Staking Supplemental Control (project) 02. 5126-00-71	LS	1.000	1.000
0450	650.9920	Construction Staking Slope Stakes	LF	200.000	200.000
0460	690.0150	Sawing Asphalt	LF	39.000	39.000
0470	715.0502	Incentive Strength Concrete Structures	DOL	1,338.000	1,338.000
0480	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	200.000	200.000
0490	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	400.000	400.000
0530	SPV.0105	Special 04. Temporary Water Diversion, Culvert B-41-304	LS	1.000	1.000

205.0100 EXCAVATION COMMON **P**
208.0100 BORROW

LOCATION	EXC. COMMON CY (3)	FILL CY (1)	EXPANDED FILL CY (2)	BORROW CY
STA 9+00 - STA 9+63	65	103	134	69
STA 10+37 - STA 11+00	62	94	123	61
UNUSABLE PAVEMENT				58
TOTALS:	127	197	257	188

- (1) - NOT A BID ITEM - FOR INFORMATIONAL PURPOSES ONLY.
(2) - FILL EXPANSION 30%
(3) - EXISTING UNUSABLE ASPHALTIC PAVEMENT IS INCLUDED IN COMMON EXCAVATION TOTALS. SEE EARTHWORK TABLE.

305.0120 BASE AGGREGATE DENSE 1 1/4-INCH
624.0100 WATER

STATION	-	STATION	1 1/4-INCH TON	WATER (1) MGAL
9+00.00	-	11+00.00	345	7
TOTALS:			345	7

- (1) - ADDITIONAL QUANTITY INCLUDED WITH EROSION CONTROL ITEMS

625.0500 SALVAGED TOPSOIL
627.0200 MULCHING
629.0210 FERTILIZER TYPE B
630.0120 SEEDING MIXTURE NO. 20
630.0200 SEEDING TEMPORARY
624.0100 WATER

STATION	-	STATION	LOCATION	SALVAGED TOPSOIL SY	MULCHING SY	FERTILIZER CWT	SEEDING #20 LB	SEEDING TEMPORARY LB	WATER (1) MGAL
9+00	-	11+00	LT	275	410	0.30	13	13	11
9+00	-	11+00	RT	290	425	0.30	14	14	12
UNDISTRIBUTED				60	85	0.05	3	3	2
TOTALS:				625	920	0.65	30	30	25

- (1) - ADDITIONAL QUANTITY INCLUDED WITH BASE AGGREGATE ITEMS.

628.1504 SILT FENCE
628.1520 SILT FENCE MAINTENANCE

STATION	-	STATION	LOCATION	FENCE LF	MAINT. LF
9+00	-	9+65	LT	80	80
9+00	-	10+25	RT	145	145
9+70	-	11+00	LT	145	145
10+33	-	11+00	RT	85	85
UNDISTRIBUTED				45	45
TOTALS:				500	500

P - PAY PLAN QUANTITY

NOTE:
ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR
ENGINEER ESTIMATE CATEGORY 0010.

628.1905 MOBILIZATIONS EROSION CONTROL
628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL

DESCRIPTION	MOBILIZATION EACH	EMERGENCY MOB. EACH
PROJECT 5126-00-71	2	2
TOTALS:	2	2

633.5100 MARKERS ROW

STATION	OFFSET	LOCATION	EACH
9+00.00	33.00	LT	1
9+00.00	33.00	RT	1
9+35.00	55.00	LT	1
9+90.00	55.00	RT	1
10+15.00	55.00	LT	1
10+60.00	55.00	RT	1
11+00.00	33.00	LT	1
11+00.00	33.00	RT	1
TOTAL:			8

633.5200 MARKERS CULVERT END

STATION	LOCATION	EACH
9+65	30' LT	1
9+99	30' LT	1
10+01	30' RT	1
10+35	30' RT	1
TOTAL:		4

NOTE:
ALL ITEMS AND QUANTITIES ON THIS SHEET ARE FOR
ENGINEER ESTIMATE CATEGORY 0010.

638.2602 REMOVING SIGNS TYPE II
638.3000 REMOVING SMALL SIGN SUPPORTS

STATION	LOCATION	REMOVING	REMOVING SMALL	COMMENTS
		SIGNS TYPE II	SIGN SUPPORTS	
		EACH	EACH	
9+58	RT	-	-	EXISTING WEIGHT LIMIT POSTING (REMOVED BY COUNTY)
9+67	LT	1	1	EXISTING OBJECT MARKER
9+84	RT	1	1	EXISTING OBJECT MARKER
10+13	LT	1	1	EXISTING OBJECT MARKER
10+26	LT	-	-	EXISTING WEIGHT LIMIT POSTING (REMOVED BY COUNTY)
10+29	RT	1	1	EXISTING OBJECT MARKER
TOTALS:		4	4	

650.4500 CONSTRUCTION STAKING SUBGRADE
650.5000 CONSTRUCTION STAKING BASE
650.9920 CONSTRUCTION STAKING SLOPE STAKES
650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 5126-00-71

STATION		STATION	SUBGRADE	BASE	SLOPE	SUPPLEMENTAL
			LF	LF	STAKES	CONTROL
					LF	LS
9+00	-	11+00	200	200	200	-
TOTALS:			200	200	200	1

690.0150 SAWING ASPHALT

STATION	LF
9+00	19
11+00	20
TOTAL:	39

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	•
QUARTER LINE	---			NON-MONUMENTED R/W POINT	o
SIXTEENTH LINE	---			FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	SECTION CORNER MONUMENT			
NEW R/W LINE	---				
EXISTING R/W OR HE LINE	---	GEODETIC SURVEY MONUMENT			
PROPERTY LINE	---	SIXTEENTH CORNER MONUMENT			
LOT, TIE & OTHER MINOR LINES	---	SIGN		OFF-PREMISE SIGN	
SLOPE INTERCEPT	---			COMPENSABLE	
CORPORATE LIMITS	---	ELECTRIC POLE		NON-COMPENSABLE	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	TELEPHONE POLE			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	ACCESS RESTRICTED BY ACQUISITION			
TRANSMISSION STRUCTURES	---	NO ACCESS (BY STATUTORY AUTHORITY)			
BUILDING TO BE REMOVED	---	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
BRIDGE	---	NO ACCESS (NEW HIGHWAY)			
		PARCEL NUMBER	25	UTILITY NUMBER	40
		PARALLEL OFFSETS			

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
CENTERLINE	C/L	EASEMENT	
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED	TLE
GAS VALVE	GV	EASEMENT	
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP
HIGHWAY EASEMENT	HE	PLAT	
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, MONROE COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT OF WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY 3" X 24" REBAR) AND ARE PLACED PRIOR TO THE COMPLETION OF THE CONSTRUCTION PROJECT.

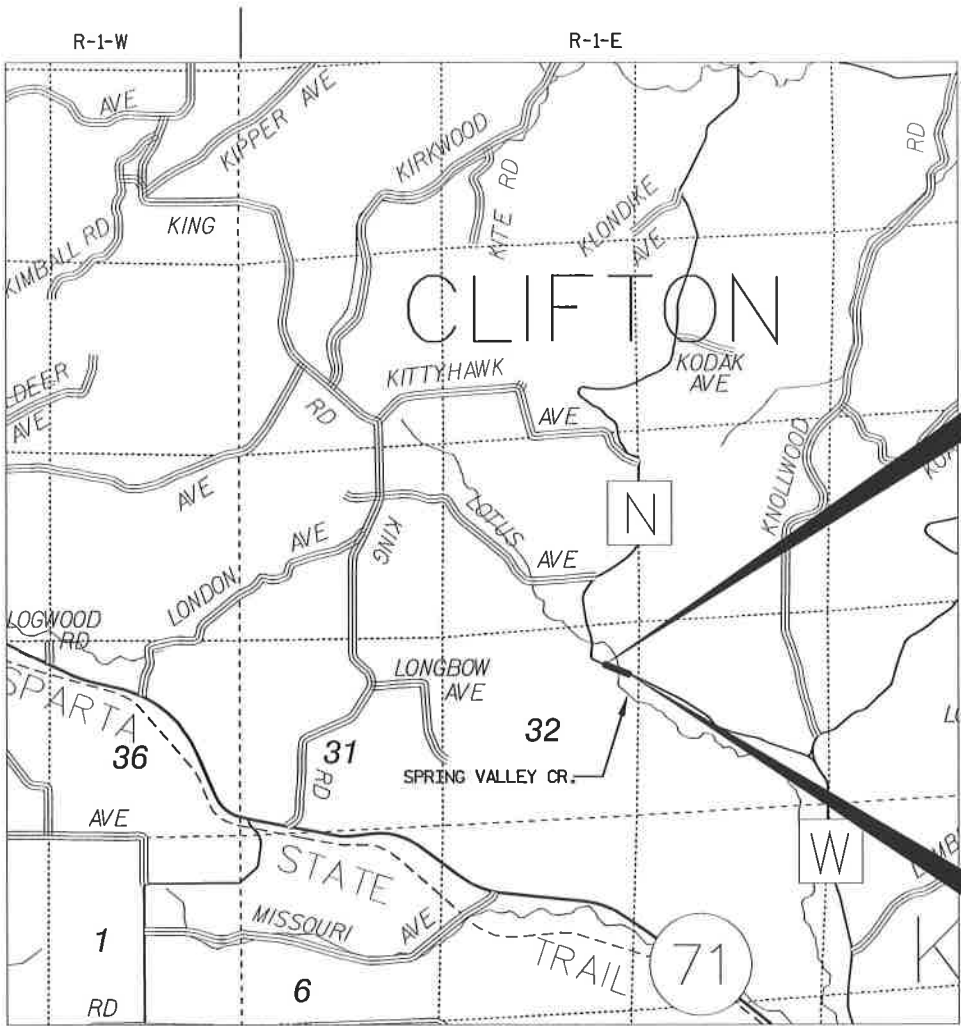
RIGHT OF WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD.

CONVENTIONAL UTILITY SYMBOLS

WATER	---
GAS	---
TELEPHONE	---
OVERHEAD	---
TRANSMISSION LINES	---
ELECTRIC	---
CABLE TELEVISION	---
FIBER OPTIC	---
SANITARY SEWER	---
STORM SEWER	---



CAUTION
THIS PLAT IS FOR ILLUSTRATIVE PURPOSES ONLY.
DEEDS MUST BE CHECKED TO DETERMINE
PROPERTY BOUNDARIES.



BEGIN RELOCATION
ORDER STA. 9+00

Y = 336,552.53
X = 736,282.26
1,317.22 FEET SOUTH OF AND 644.04 FEET
WEST OF THE NORTHEAST CORNER OF
SECTION 32, T-16-N, R-1-E, TOWN OF
CLIFTON, MONROE COUNTY, WI

END RELOCATION
ORDER STA. 11+00

Y = 336,479.27
X = 736,468.36
1,390.48 FEET SOUTH OF AND 457.94 FEET
WEST OF THE NORTHEAST CORNER OF
SECTION 32, T-16-N, R-1-E, TOWN OF
CLIFTON, MONROE COUNTY, WI

LAYOUT
SCALE 0 1 MILE
TOTAL NET LENGTH OF CENTERLINE = 0.038

R/W PROJECT NUMBER 5126-00-01	SHEET NUMBER	TOTAL SHEETS
R/W PROJECT NUMBER	4.01	2
PLAT OF RIGHT OF WAY REQUIRED FOR GLENDALE - CLIFTON (SPRING VALLEY CREEK BRIDGE B-41-0304) CTH N MONROE COUNTY CONSTRUCTION PROJECT NUMBER: 5126-00-71		

RECEIVED
FEB 26 2016
MONROE COUNTY CLERK

ORIGINAL PLAT PREPARED BY

MSA

PROFESSIONAL SERVICES

TRANSPORTATION • MUNICIPAL

DEVELOPMENT • ENVIRONMENTAL

1230 South Boulevard Baraboo, WI 53913

608-356-2771 1-800-362-4505 Fax: 608-356-2770

Web Address: www.msa-ps.com

© MSA Professional Services, Inc.



1/12/2016
(Date)

Gregory P. Rhinehart
(Professional Land Surveyor)

REVISION DATE

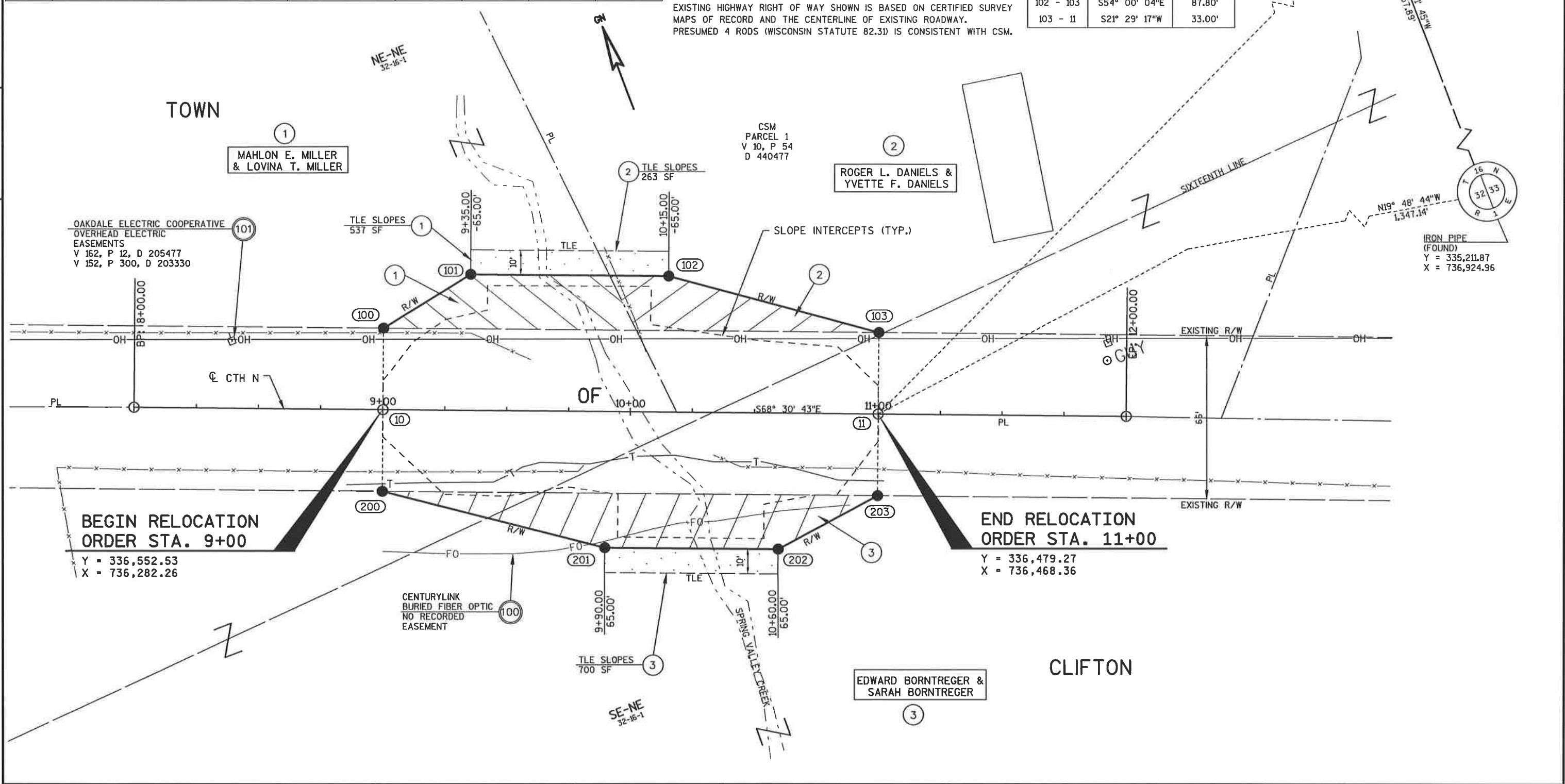
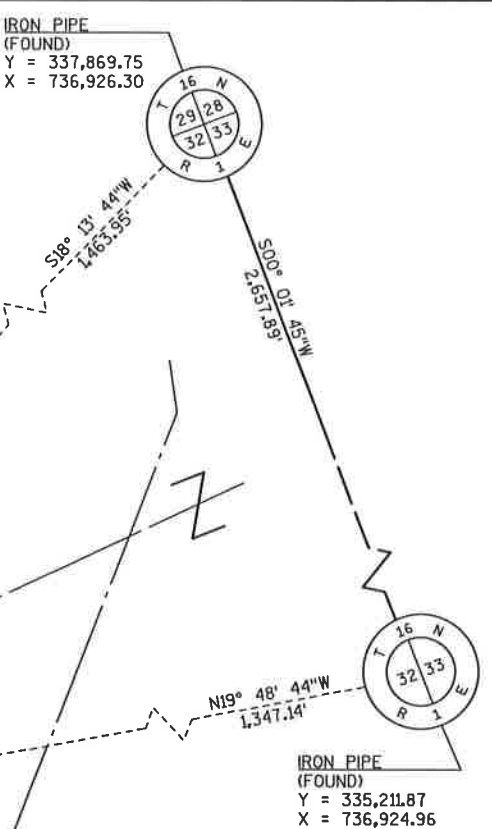
APPROVED FOR MONROE COUNTY

DATE: 02/16/2016
(Signature)
C. H. H. Commissioner

SCHEDULE OF LANDS & INTERESTS REQUIRED						
OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE COUNTY. ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED.						
PARCEL NUMBER	OWNER(S) NAME	INTEREST REQUIRED	FEE	EXISTING R/W	TOTAL FEE	TLE
1	MAHLON E. MILLER & LOVINA T. MILLER	FEE & TLE	0.04	0.08	0.12	0.01
2	ROGER L. DANIELS & YVETTE F. DANIELS	FEE & TLE	0.03	0.07	0.10	0.01
3	EDWARD BORNTREGER & SARAH BORNTREGER	FEE & TLE	0.07	0.15	0.22	0.02
100	CENTURYLINK	REL. OF RIGHTS	-	-	-	-
101	OAKDALE ELECTRIC COOPERATIVE	REL. OF RIGHTS	-	-	-	-

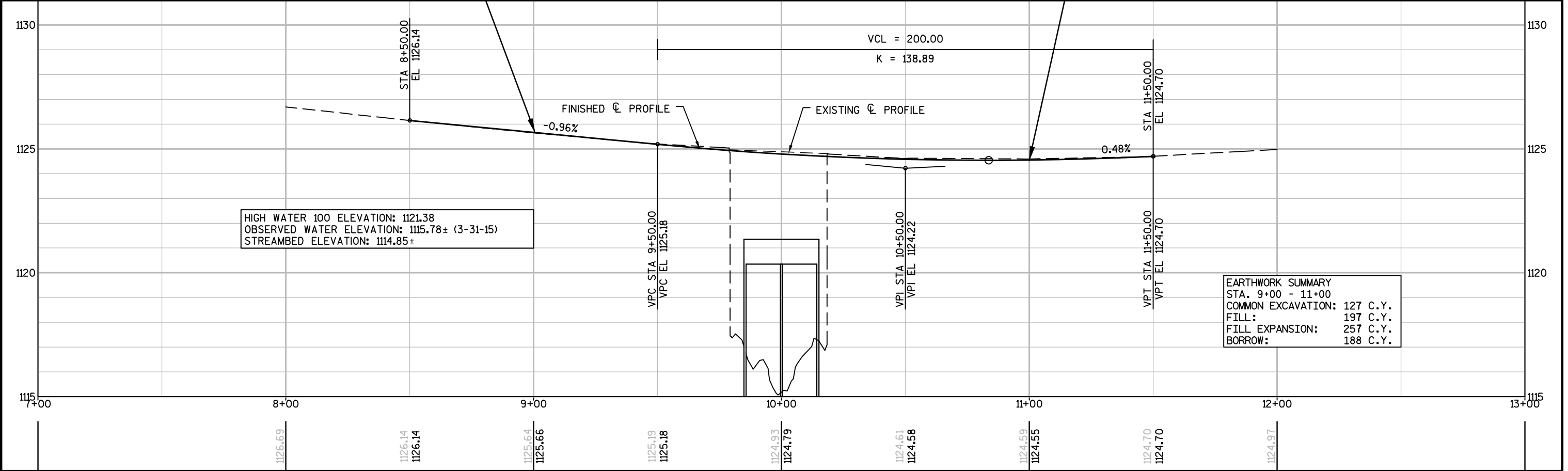
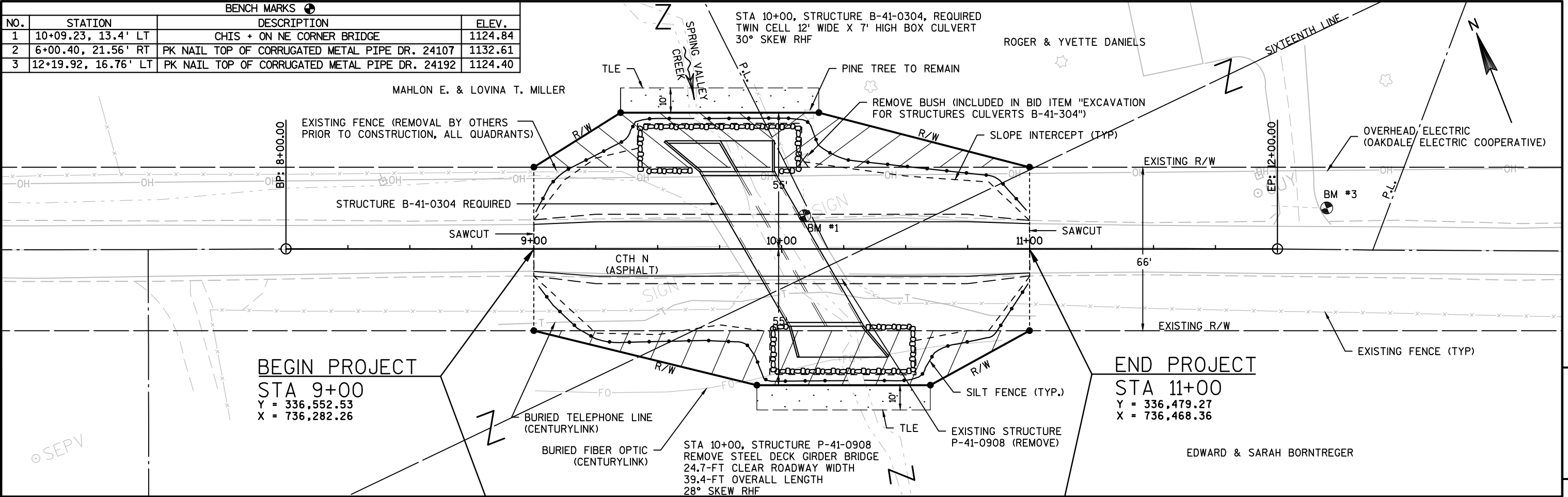
PT.	STATION	OFFSET	Y	X
100	9+00.00	33.00' LT.	336,583.24	736,294.35
101	9+35.00	55.00' LT.	336,590.89	736,334.97
102	10+15.00	55.00' LT.	336,561.58	736,409.41
103	11+00.00	33.00' LT.	336,509.97	736,480.45
200	9+00.00	33.00' RT.	336,521.82	736,270.17
201	9+90.00	55.00' RT.	336,468.38	736,345.86
202	10+60.00	55.00' RT.	336,442.74	736,410.99
203	11+00.00	33.00' RT.	336,448.56	736,456.27

PT - PT	BEARING	DISTANCE
11 - 203	S21° 29' 17"W	33.00'
203 - 202	S82° 40' 38"W	45.65'
202 - 201	N68° 30' 43"W	70.00'
201 - 200	N54° 46' 33"W	92.65'
200 - 10	N21° 29' 17"E	33.00'
10 - 100	N21° 29' 17"E	33.00'
100 - 101	N79° 20' 08"E	41.34'
101 - 102	S68° 30' 43"E	80.00'
102 - 103	S54° 00' 04"E	87.80'
103 - 11	S21° 29' 17"W	33.00'



REVISION DATE	DATE : 1/12/2016	SCALE, FEET	HWY: CTH N	STATE R/W PROJECT NUMBER 5126-00-01	PLAT SHEET 4.02
	GRID FACTOR	0 20 40	COUNTY: MONROE	CONSTRUCTION PROJECT NUMBER 5126-00-71	PS&E SHEET

BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
1	10+09.23, 13.4' LT	CHIS + ON NE CORNER BRIDGE	1124.84
2	6+00.40, 21.56' RT	PK NAIL TOP OF CORRUGATED METAL PIPE DR. 24107	1132.61
3	12+19.92, 16.76' LT	PK NAIL TOP OF CORRUGATED METAL PIPE DR. 24192	1124.40



Standard Detail Drawing List

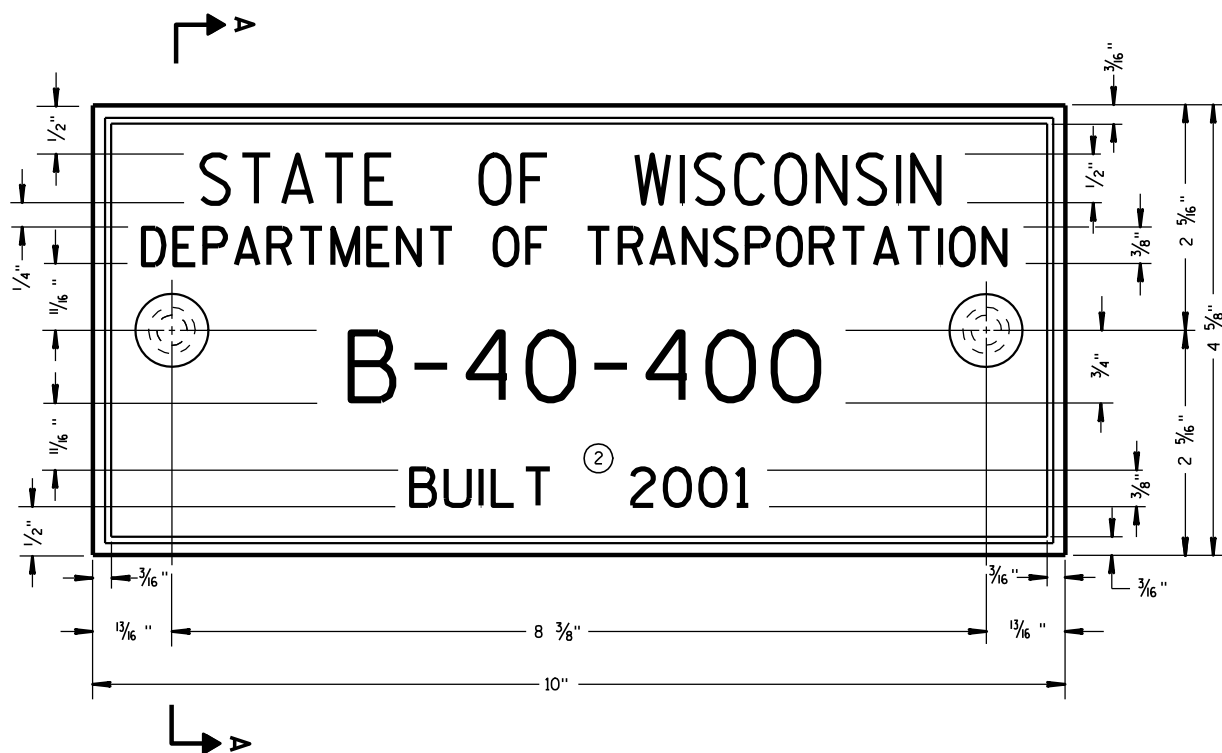
08E09-06	SILT FENCE
12A03-10	NAME PLATE (STRUCTURES)
15A01-12A	MARKER POST FOR RIGHT-OF-WAY
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-06A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-06B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES



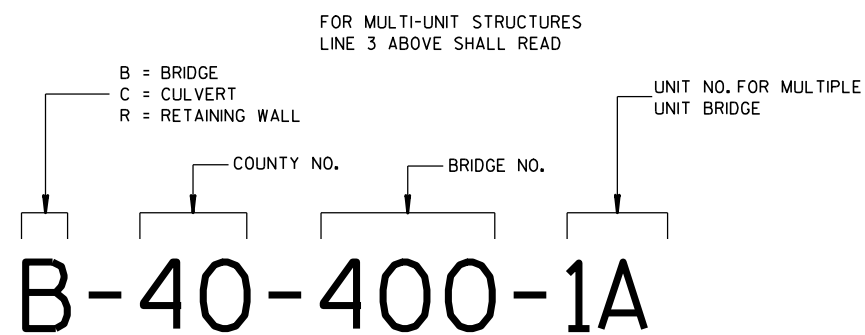
- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1½" X 1½" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



<p>SILT FENCE</p>	
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>	
<p>APPROVED 4-29-05 DATE</p>	<p>/s/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER</p>



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)



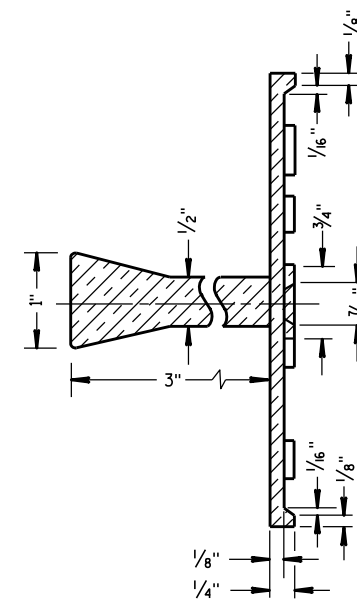
**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

GENERAL NOTES

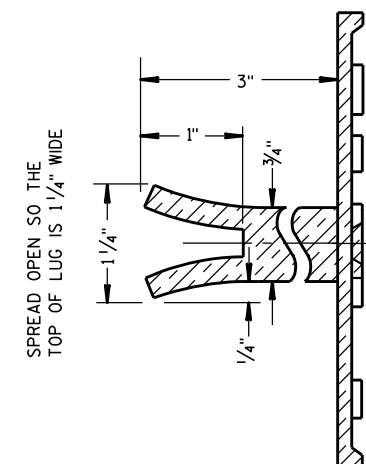
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

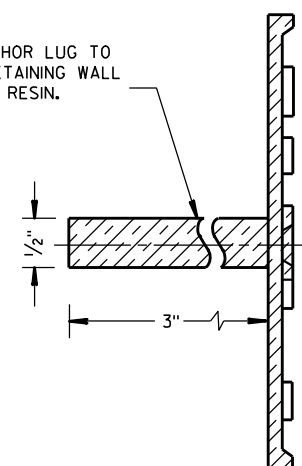


SECTION A-A



ALTERNATE LUG

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

**NAME PLATE
(STRUCTURES)**

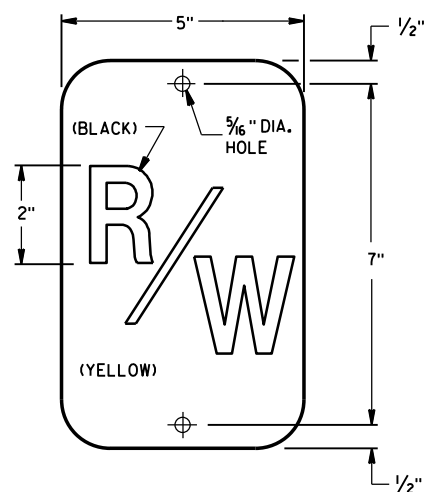
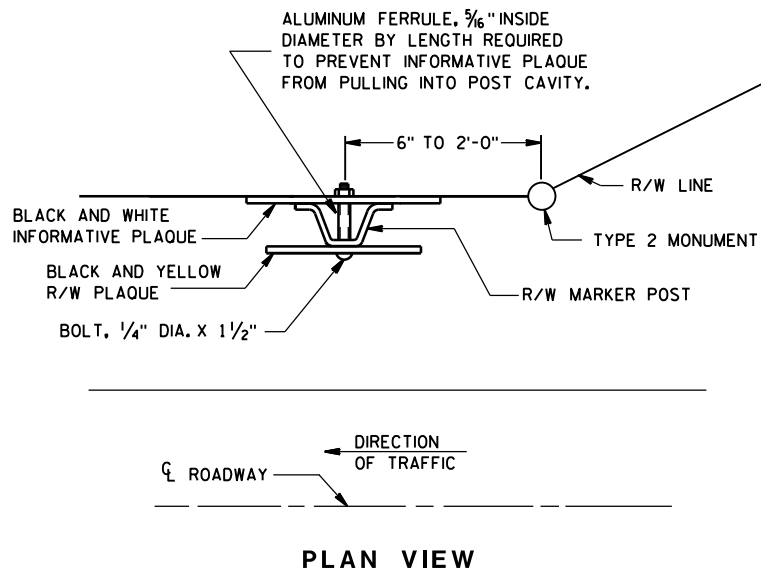
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

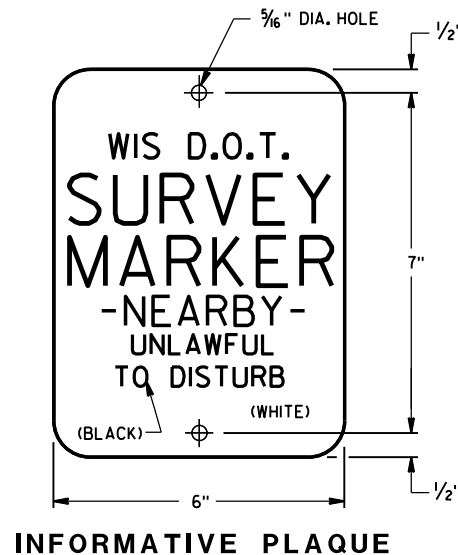
3/26/10
DATE

FHWA

/S/ Scot Becker
CHIEF STRUCTURAL DEVELOPMENT ENGINEER



THE RIGHT-OF-WAY PLAQUE AND INFORMATIVE PLAQUE WILL BE FURNISHED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION.



GENERAL NOTES

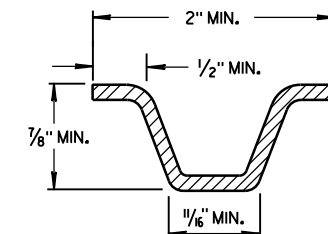
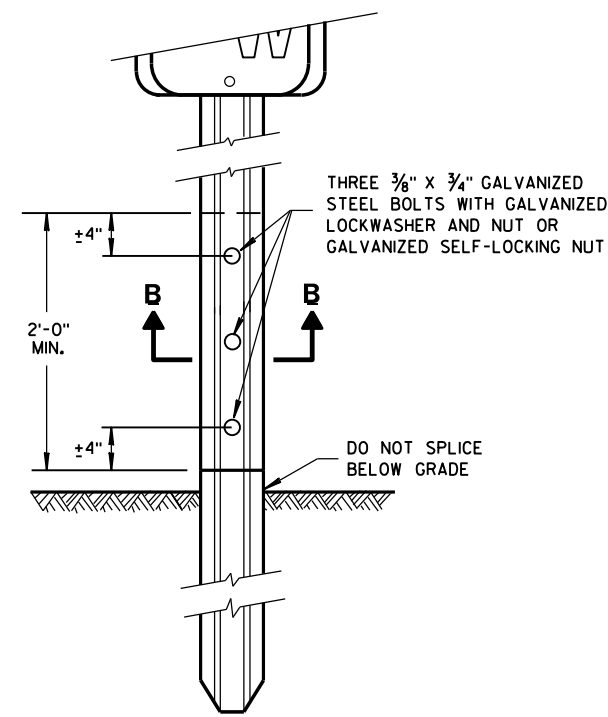
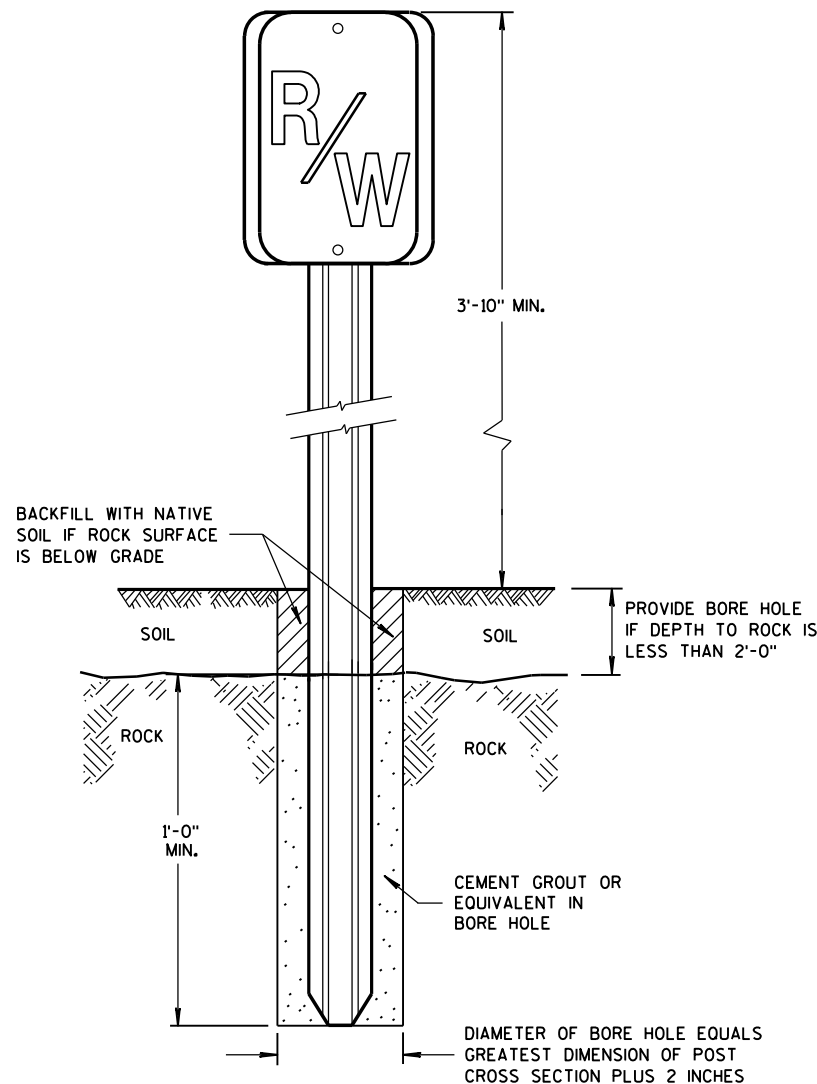
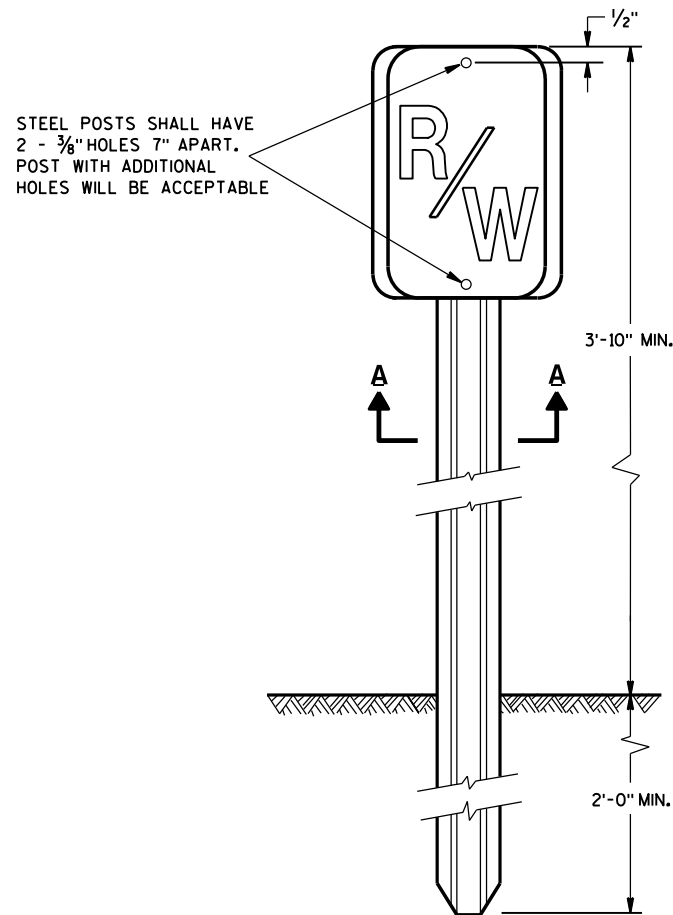
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

A STEEL MARKER POST FOR RIGHT-OF-WAY SHALL BE PLACED IN THE RIGHT-OF-WAY, WITH THE BACK OF THE POST ON THE LONGER RIGHT-OF-WAY TANGENT, 6 INCHES TO 24 INCHES FROM EACH TYPE 2 MONUMENT TO SERVE AS A GUARD POST, AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

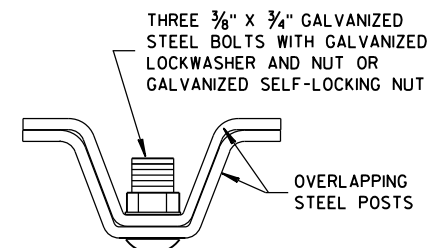
THE "R/W" PLAQUE SHALL FACE THE ROADWAY AND THE INFORMATIVE PLAQUE SHALL FACE AWAY FROM THE ROADWAY. R/W AND INFORMATIVE PLAQUES WILL BE FURNISHED BY THE DEPARTMENT OF TRANSPORTATION.

STEEL MARKER POSTS SHALL MEET THE MINIMUM MATERIAL REQUIREMENTS FOR STEEL DELINEATOR POSTS; EXCEPT POSTS PAINTED WITH FEDERAL YELLOW ENAMEL NEED NOT BE ZINC COATED.

- ① IN AREAS OF SOLID ROCK, DRILL A BORE HOLE 2" GREATER THAN THE WIDEST DIMENSION OF THE POST CROSS SECTION INTO THE ROCK TO A MINIMUM DEPTH OF 12 INCHES. CUT OR SPLICE THE POST SO THAT A MINIMUM LENGTH OF 3' 10" PROTRUDES ABOVE THE GROUND. BLOW OUT THE BORE HOLE IN THE ROCK USING COMPRESSED AIR. FILL THE BORE HOLE WITH CEMENT GROUT, OR EQUIVALENT, DEPENDING ON THE STABILITY OF THE ROCK.



MIN. WEIGHT 1.12 LB./FT.
SECTION A-A



SECTION B-B

**MARKER POST
FOR RIGHT-OF-WAY**

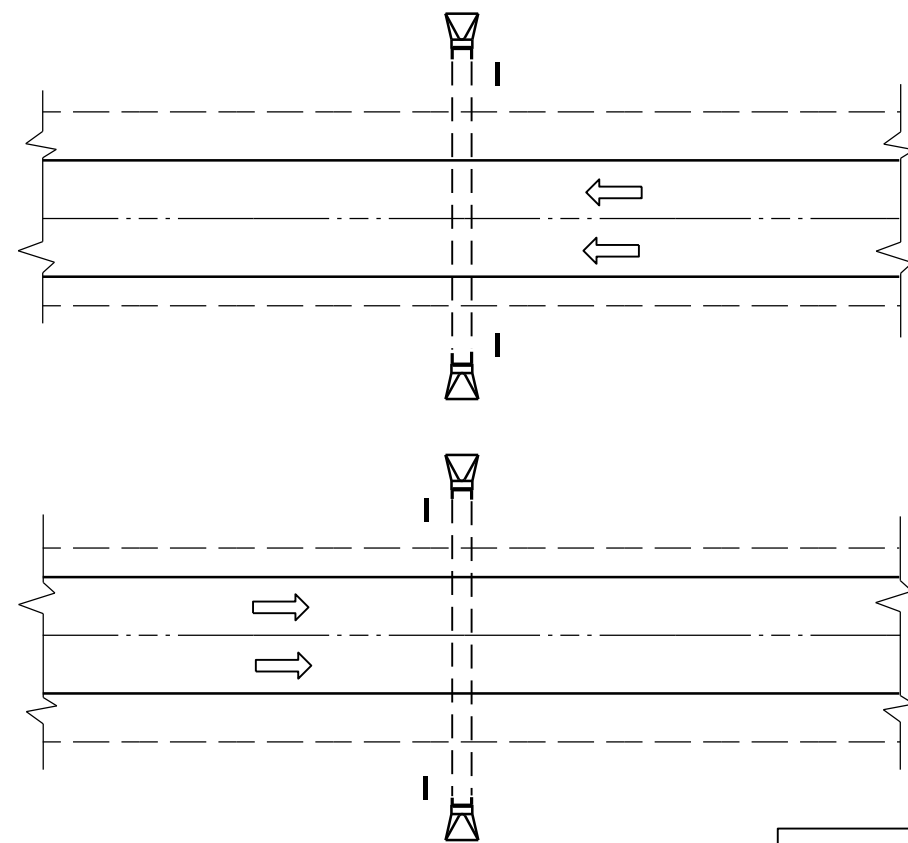
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

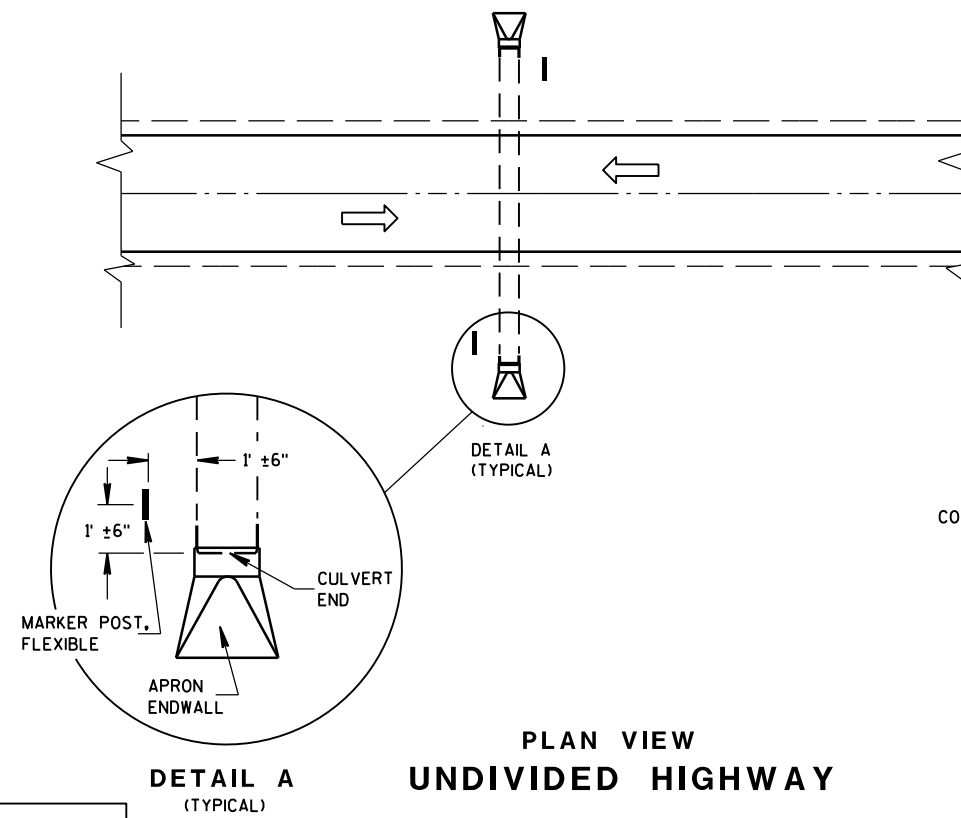
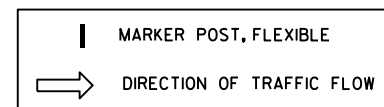
June, 2015
DATE

/S/ Ray Kumapayi
CHIEF SURVEYING AND MAPPING ENGINEER

FHWA



PLAN VIEW
DIVIDED HIGHWAY

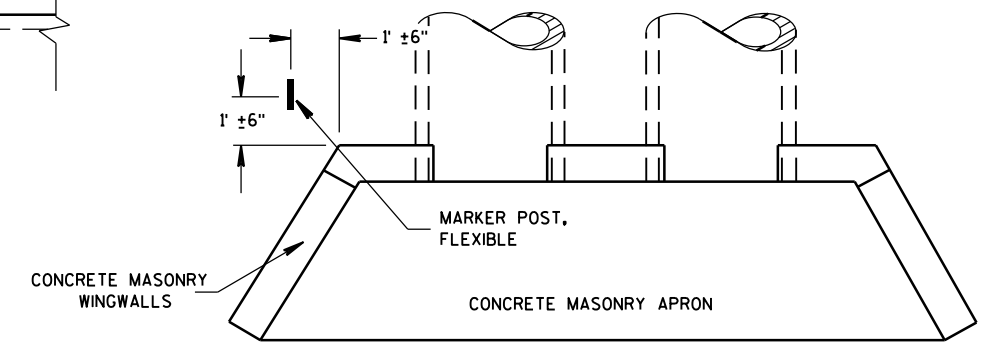


PLAN VIEW
UNDIVIDED HIGHWAY

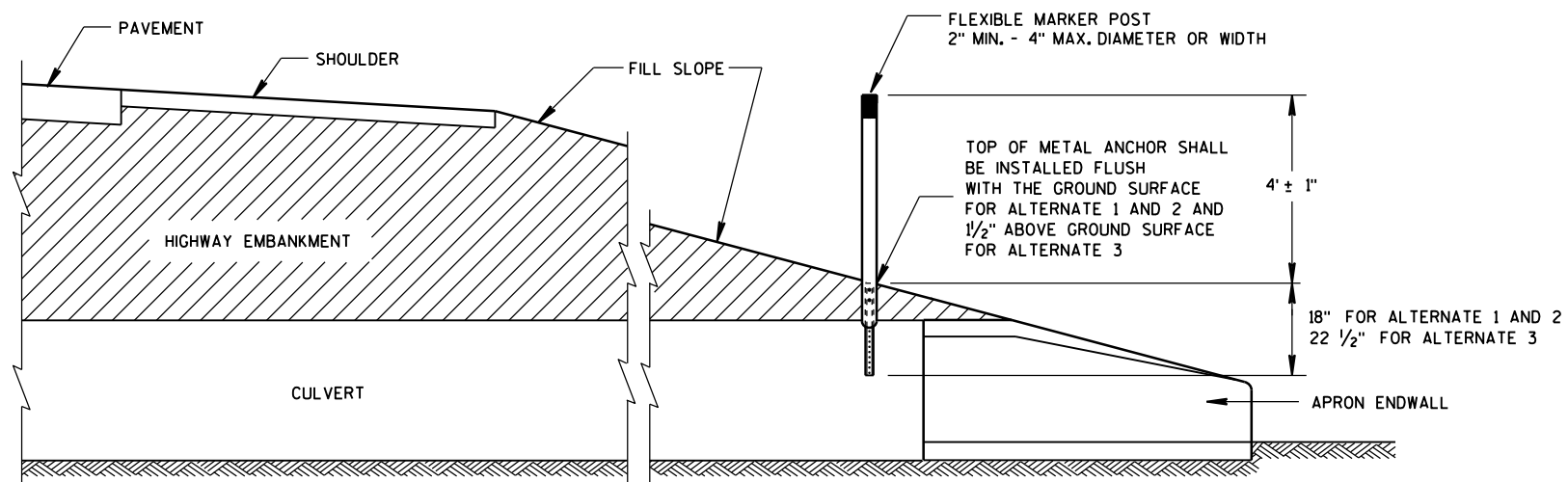
FLEXIBLE MARKER POST LOCATION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



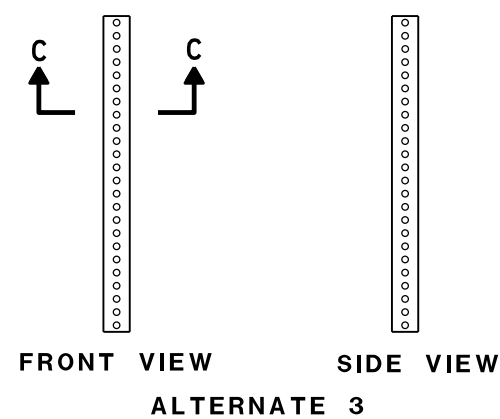
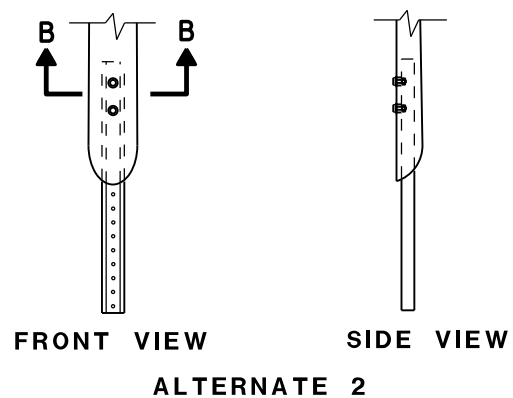
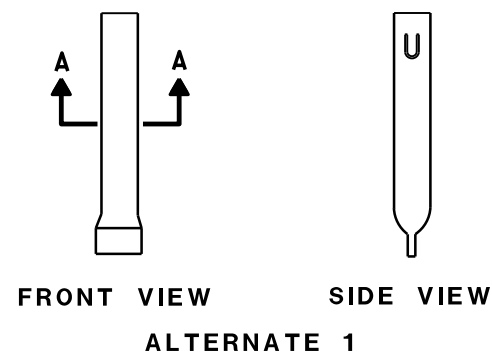
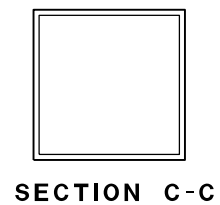
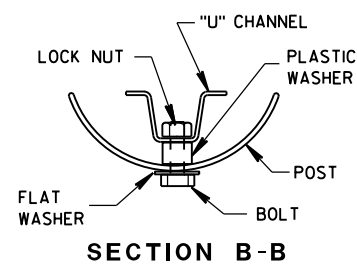
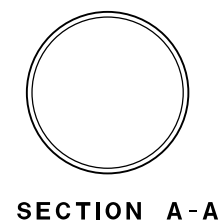
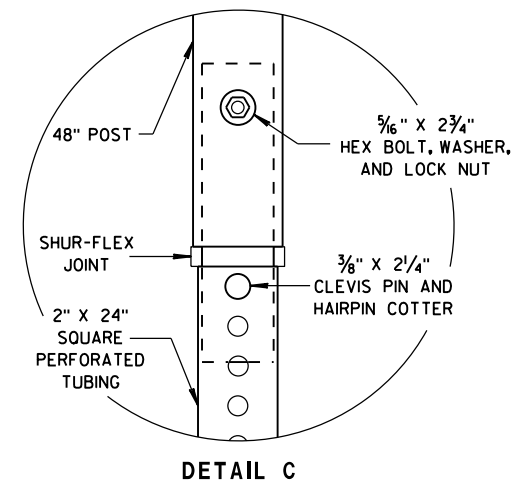
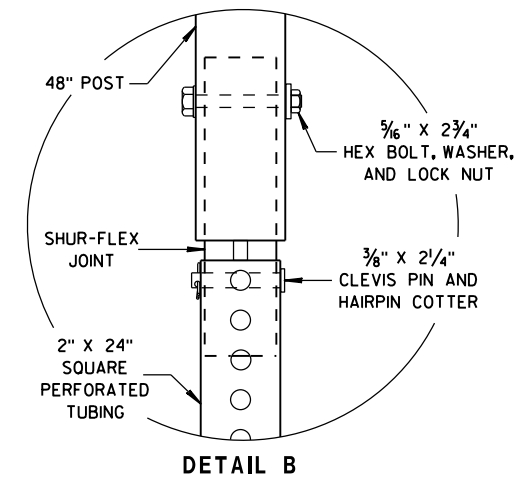
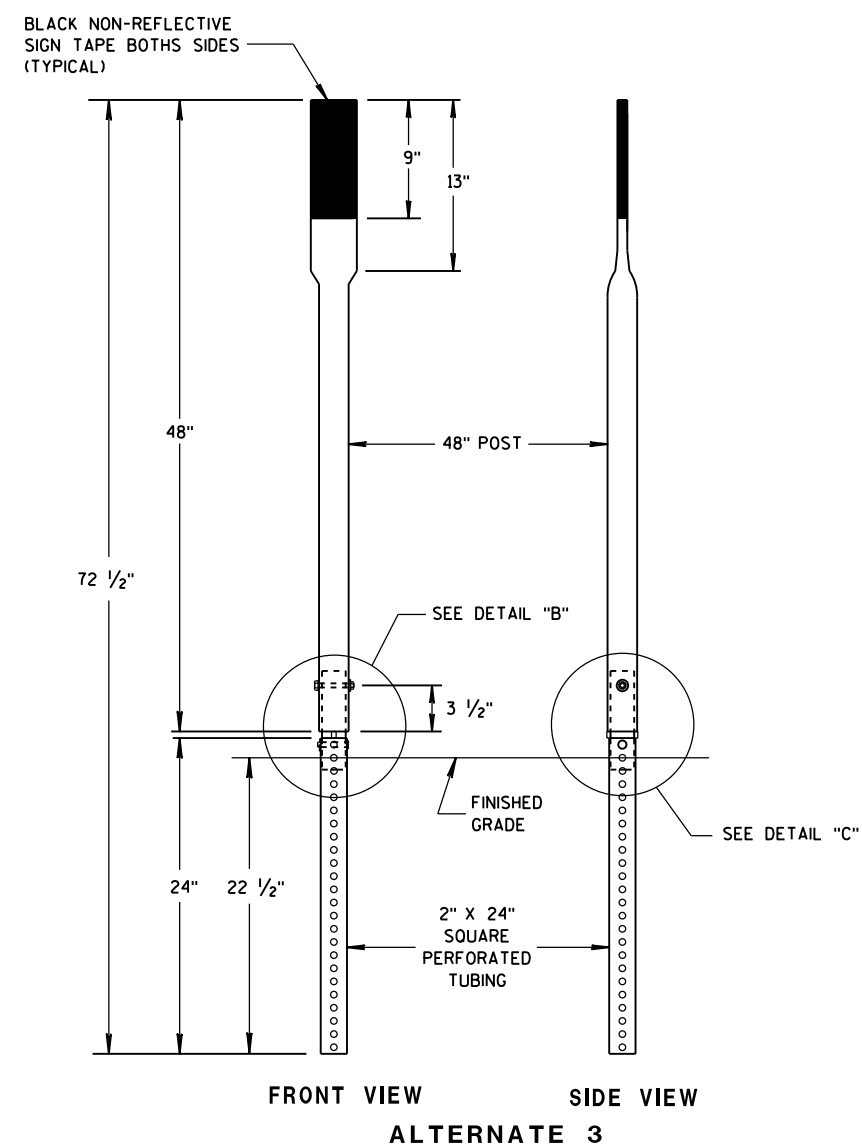
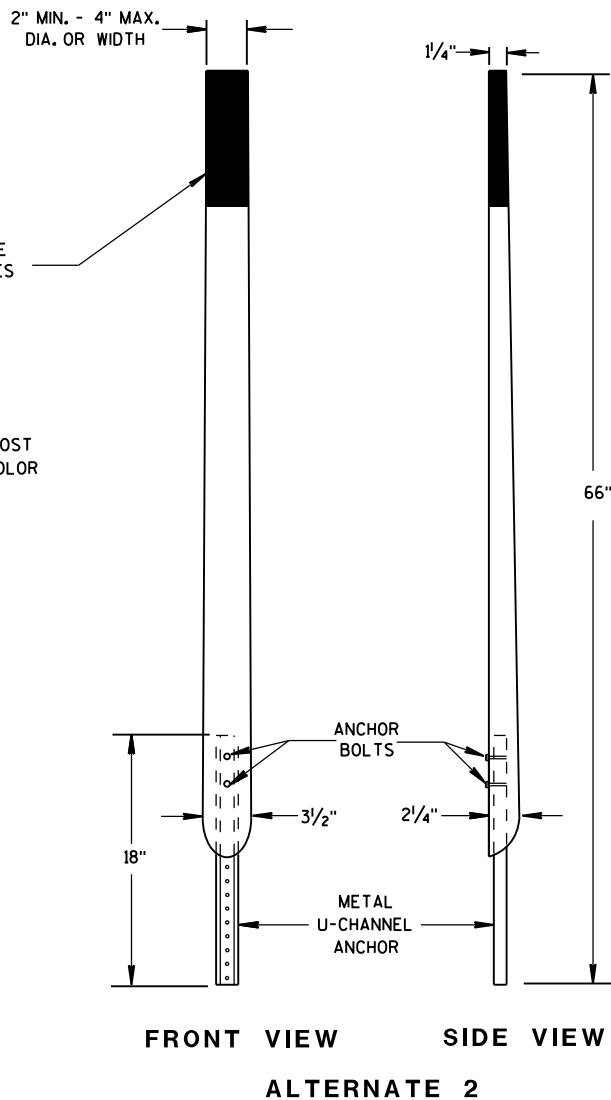
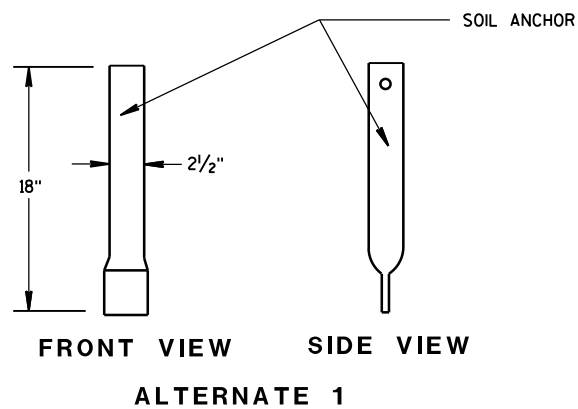
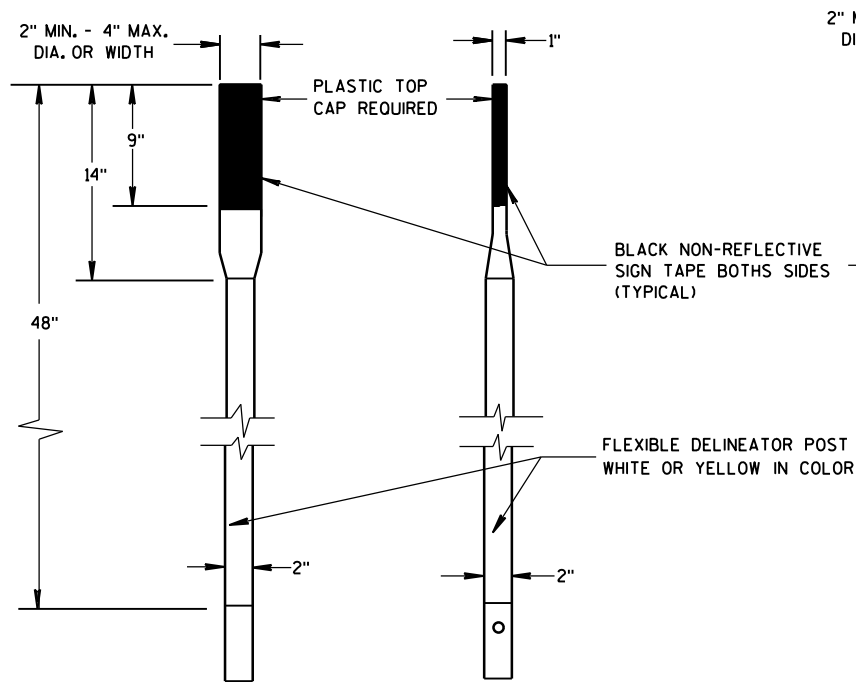
PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH



CROSS SECTION
FLEXIBLE MARKER POST

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

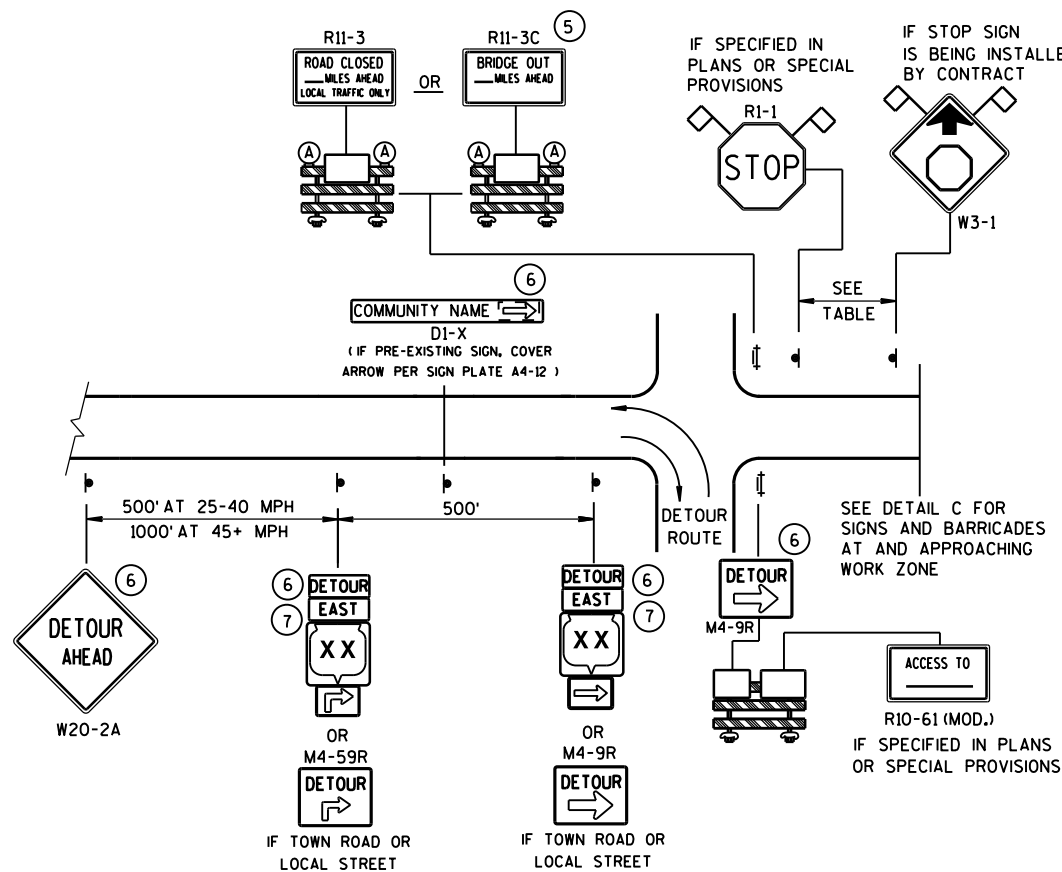


FLEXIBLE MARKER POST FOR CULVERT END

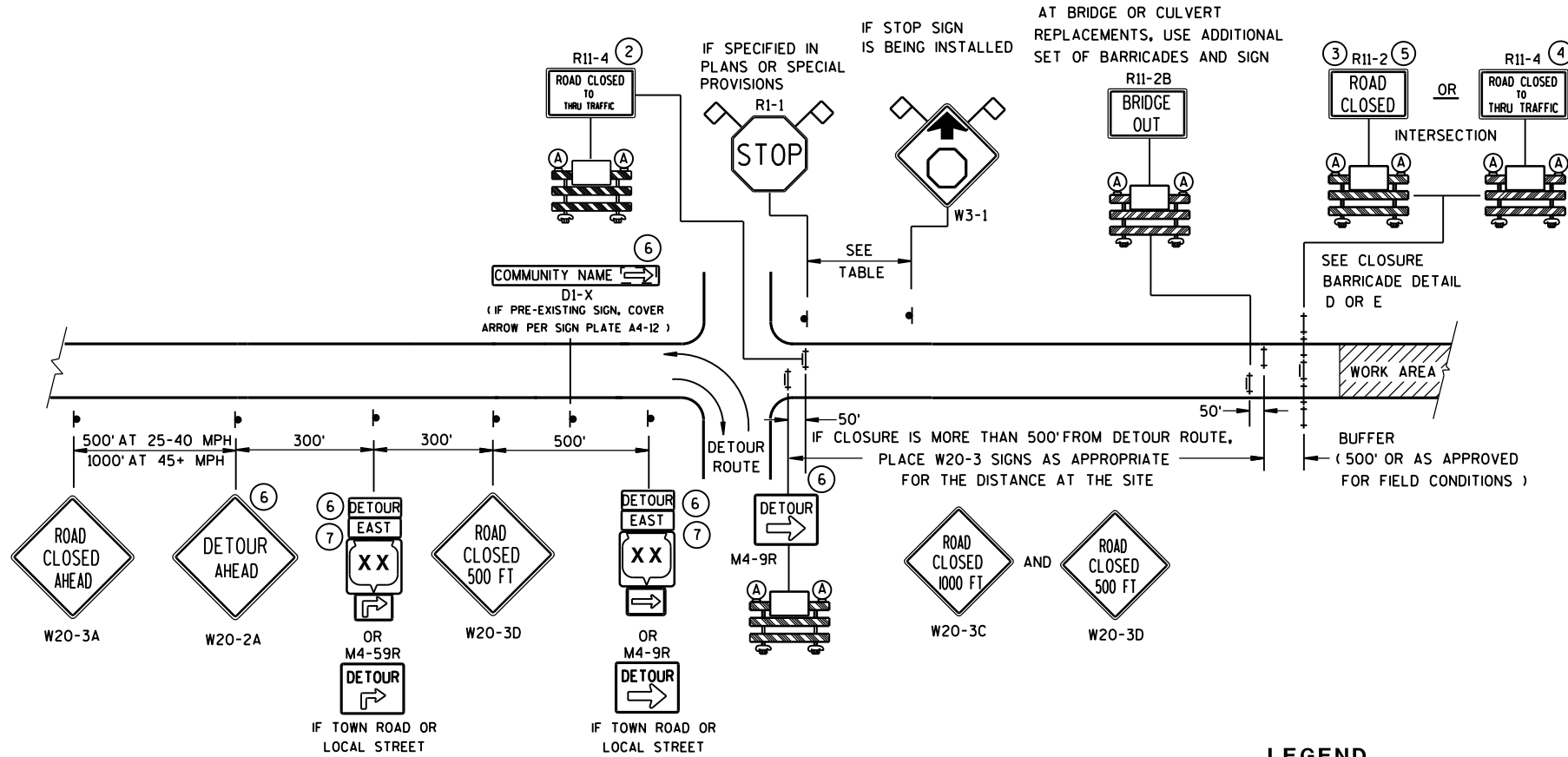
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012
DATE
FHWA

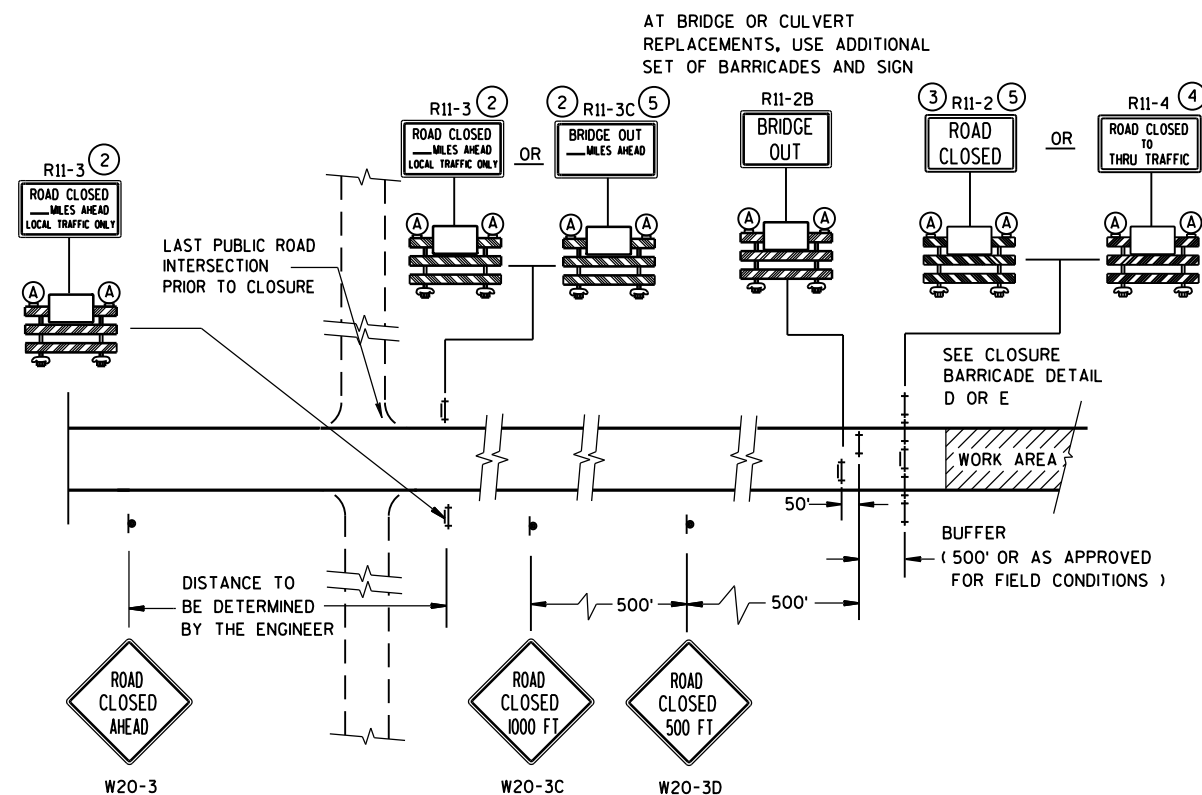
/S/ Travis Feltes
STATE TRAFFIC ENGINEER OF DESIGN



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

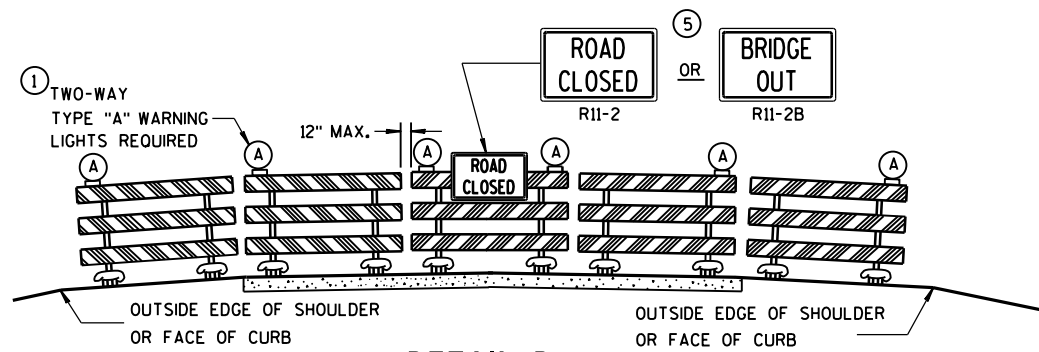


DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

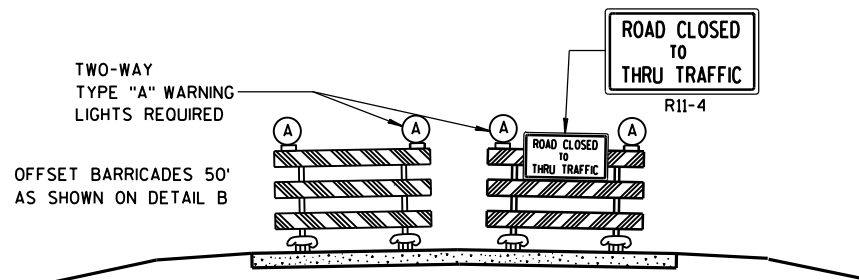
SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Sept. 2015 DATE	/S/ Peter Amokobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

- 1 TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- 2 THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- 3 FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- 4 FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- 5 FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- 6 INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- 7 "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

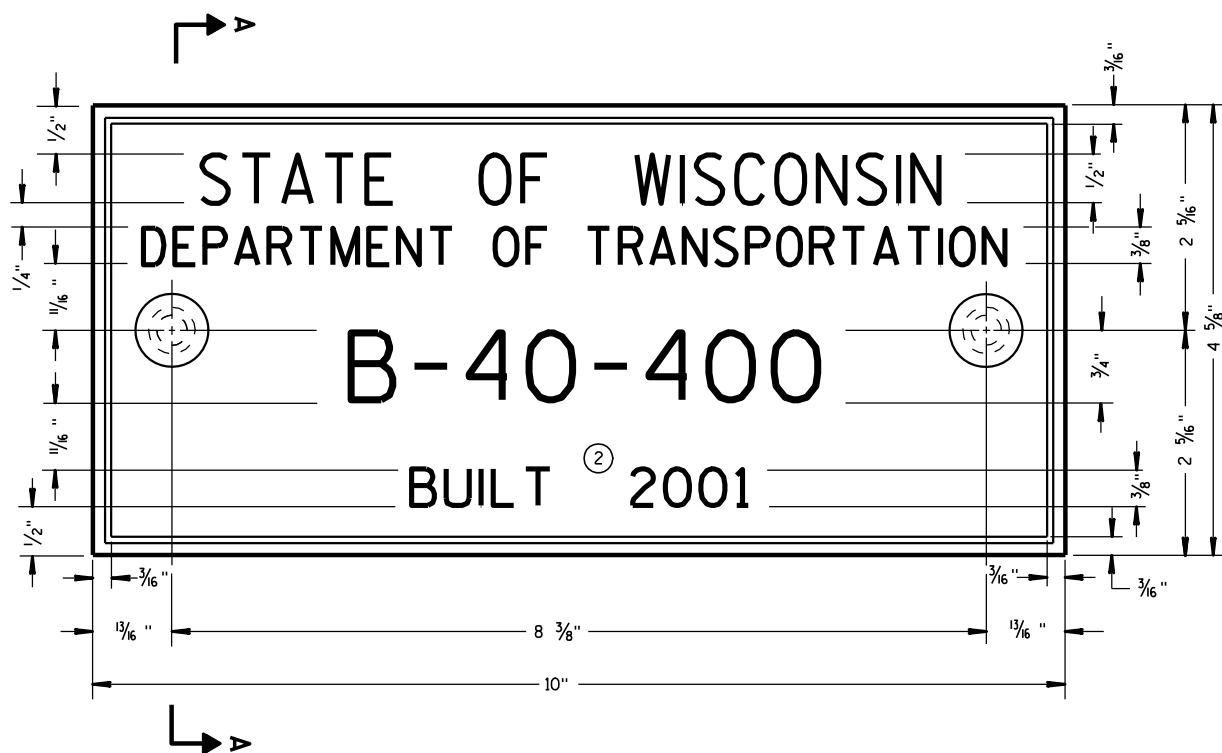
BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
Sept. 2015 DATE	/S/ Peter Amokobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	



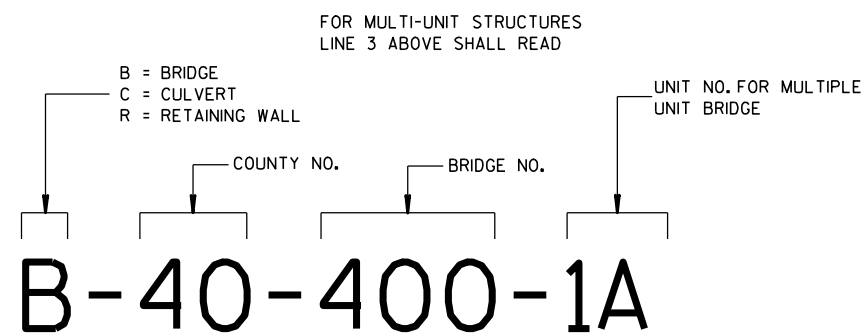
- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1½" X 1½" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



<p>SILT FENCE</p>	
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>	
<p>APPROVED 4-29-05 DATE</p>	<p>/s/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER</p>



TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)



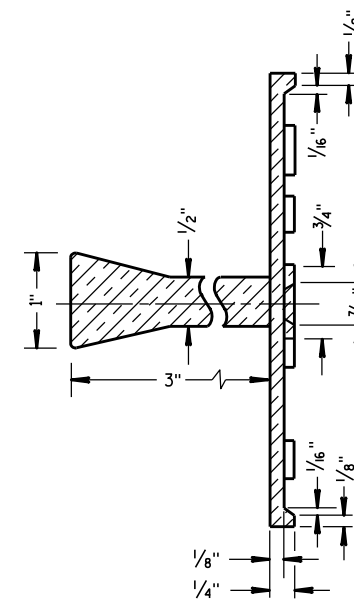
**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

GENERAL NOTES

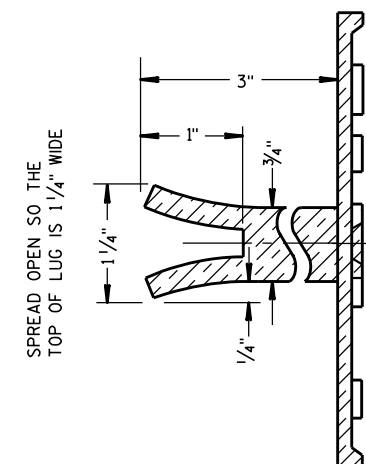
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

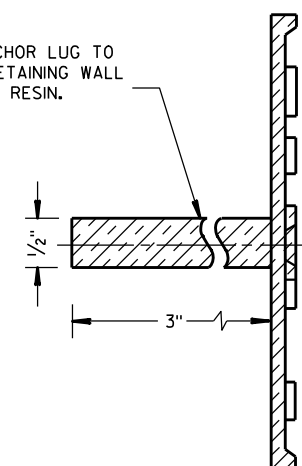


SECTION A-A



ALTERNATE LUG

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.



ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

**NAME PLATE
(STRUCTURES)**

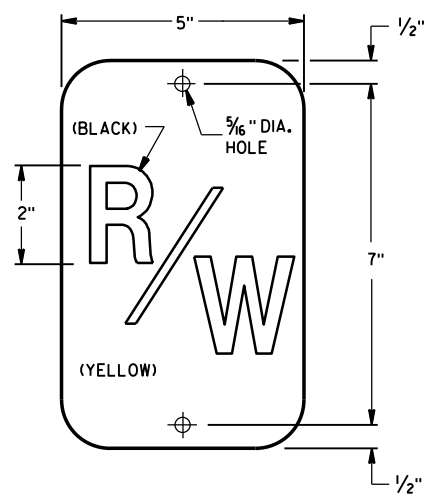
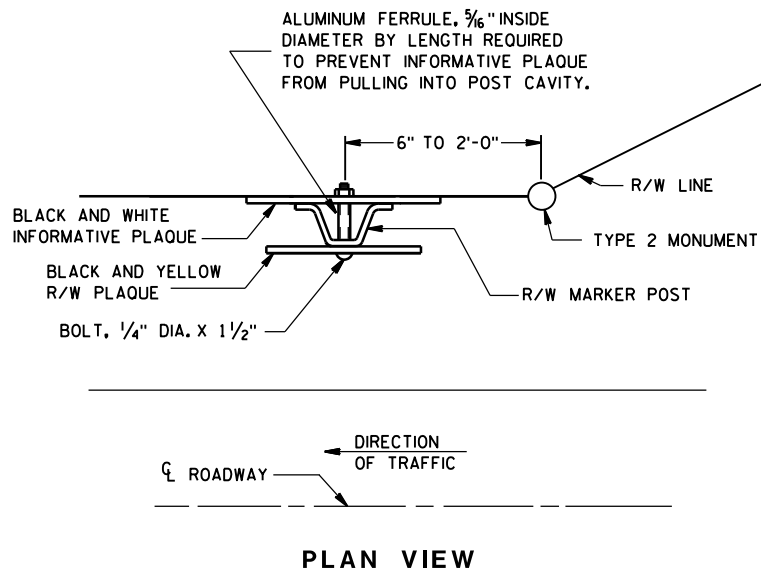
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

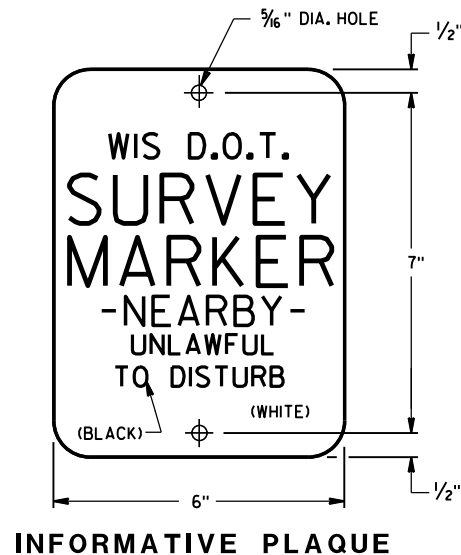
3/26/10
DATE

FHWA

/S/ Scot Becker
CHIEF STRUCTURAL DEVELOPMENT ENGINEER



R/W PLAQUE
THE RIGHT-OF-WAY PLAQUE AND INFORMATIVE PLAQUE WILL BE FURNISHED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION.



GENERAL NOTES

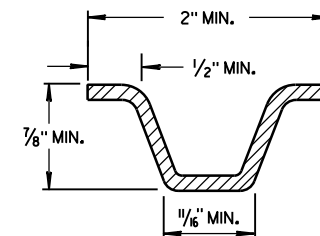
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

A STEEL MARKER POST FOR RIGHT-OF-WAY SHALL BE PLACED IN THE RIGHT-OF-WAY, WITH THE BACK OF THE POST ON THE LONGER RIGHT-OF-WAY TANGENT, 6 INCHES TO 24 INCHES FROM EACH TYPE 2 MONUMENT TO SERVE AS A GUARD POST, AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

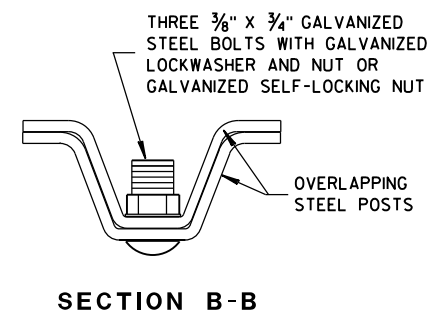
THE "R/W" PLAQUE SHALL FACE THE ROADWAY AND THE INFORMATIVE PLAQUE SHALL FACE AWAY FROM THE ROADWAY. R/W AND INFORMATIVE PLAQUES WILL BE FURNISHED BY THE DEPARTMENT OF TRANSPORTATION.

STEEL MARKER POSTS SHALL MEET THE MINIMUM MATERIAL REQUIREMENTS FOR STEEL DELINEATOR POSTS; EXCEPT POSTS PAINTED WITH FEDERAL YELLOW ENAMEL NEED NOT BE ZINC COATED.

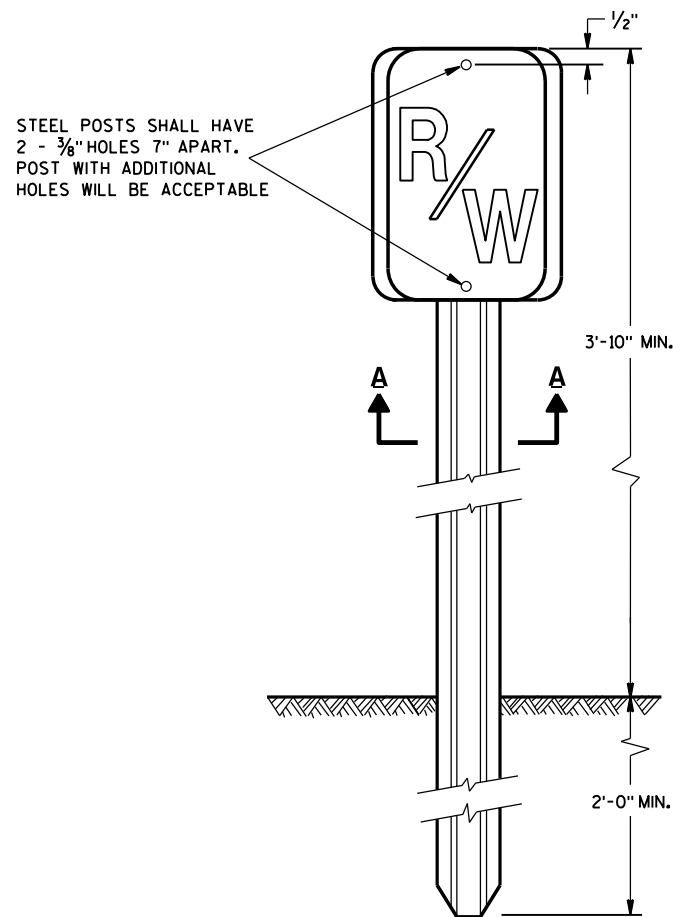
- ① IN AREAS OF SOLID ROCK, DRILL A BORE HOLE 2" GREATER THAN THE WIDEST DIMENSION OF THE POST CROSS SECTION INTO THE ROCK TO A MINIMUM DEPTH OF 12 INCHES. CUT OR SPLICE THE POST SO THAT A MINIMUM LENGTH OF 3' 10" PROTRUDES ABOVE THE GROUND. BLOW OUT THE BORE HOLE IN THE ROCK USING COMPRESSED AIR. FILL THE BORE HOLE WITH CEMENT GROUT, OR EQUIVALENT, DEPENDING ON THE STABILITY OF THE ROCK.



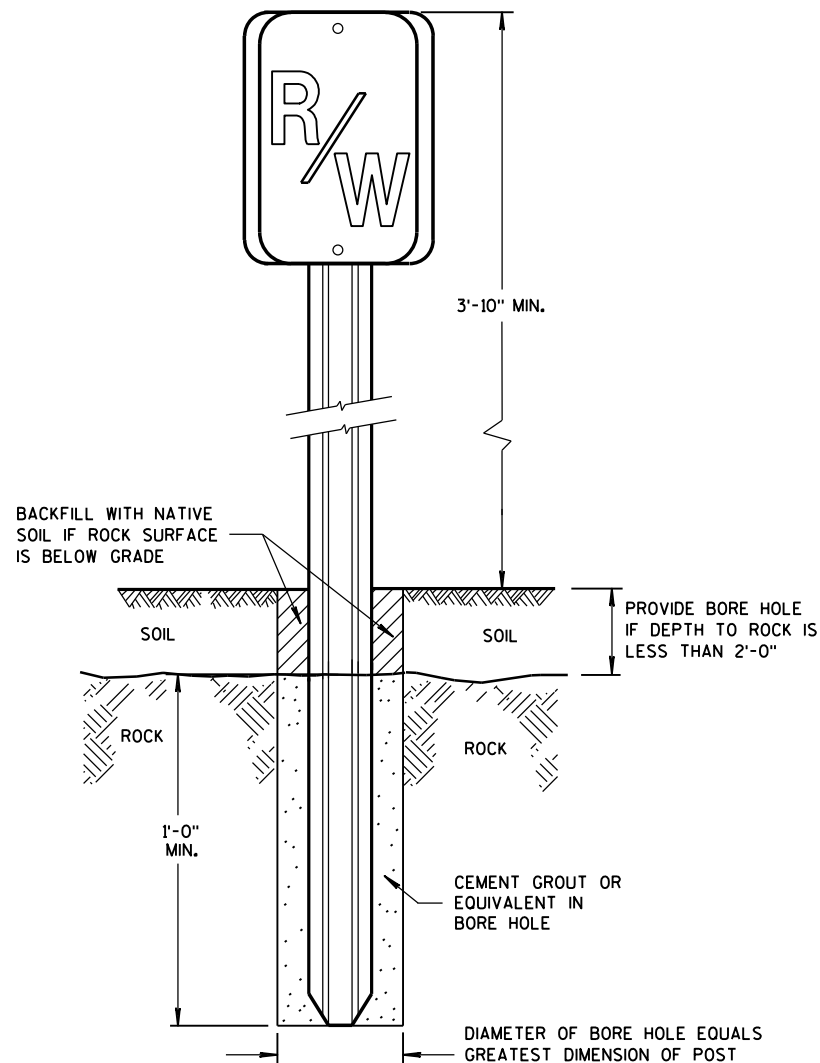
MIN. WEIGHT 1.12 LB./FT.
SECTION A-A



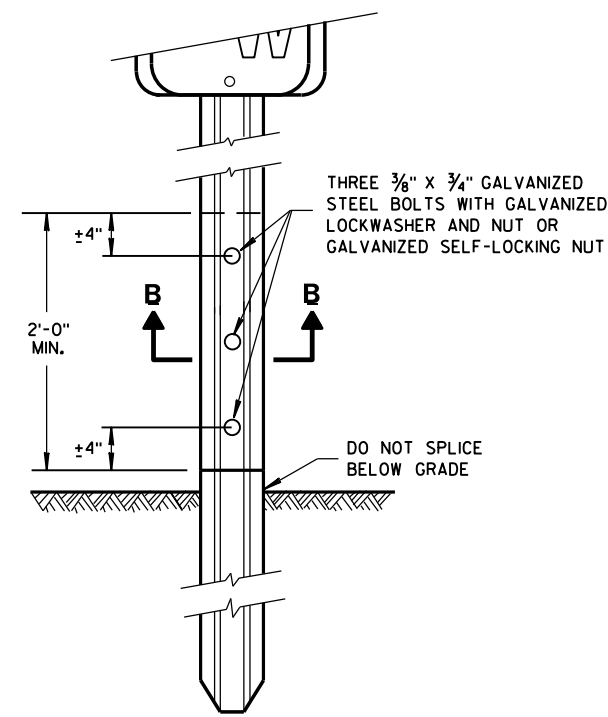
SECTION B-B



**FRONT VIEW
STEEL MARKER POST**



**FRONT VIEW
ROCK INSTALLATION** ①



**FRONT VIEW
SPLICE DETAIL**

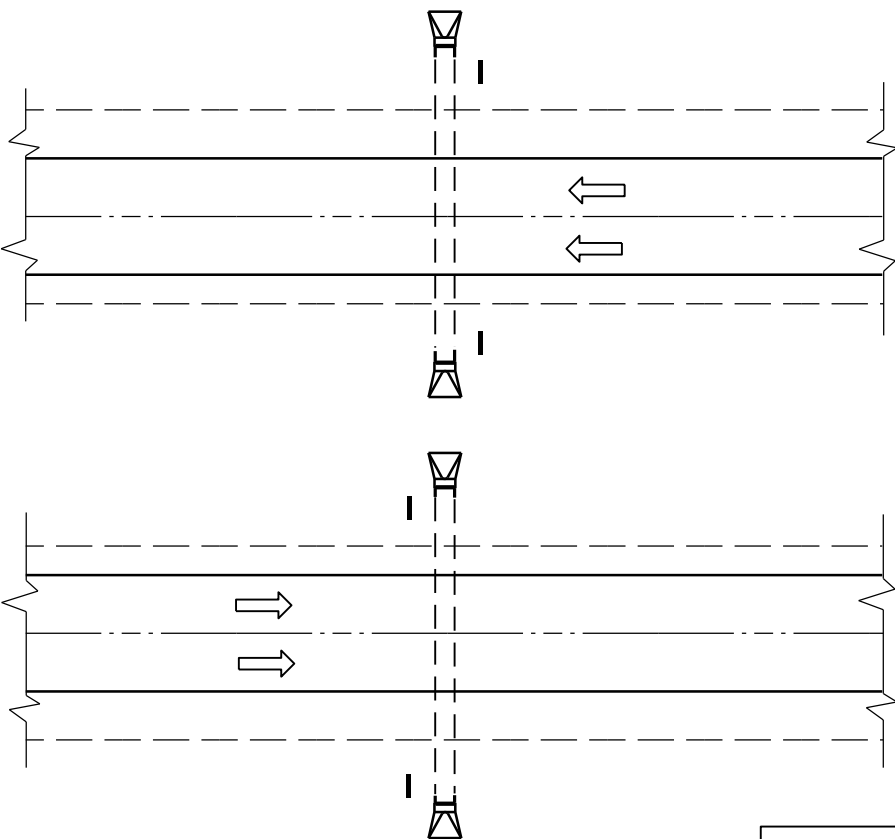
**MARKER POST
FOR RIGHT-OF-WAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

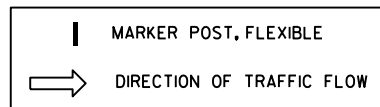
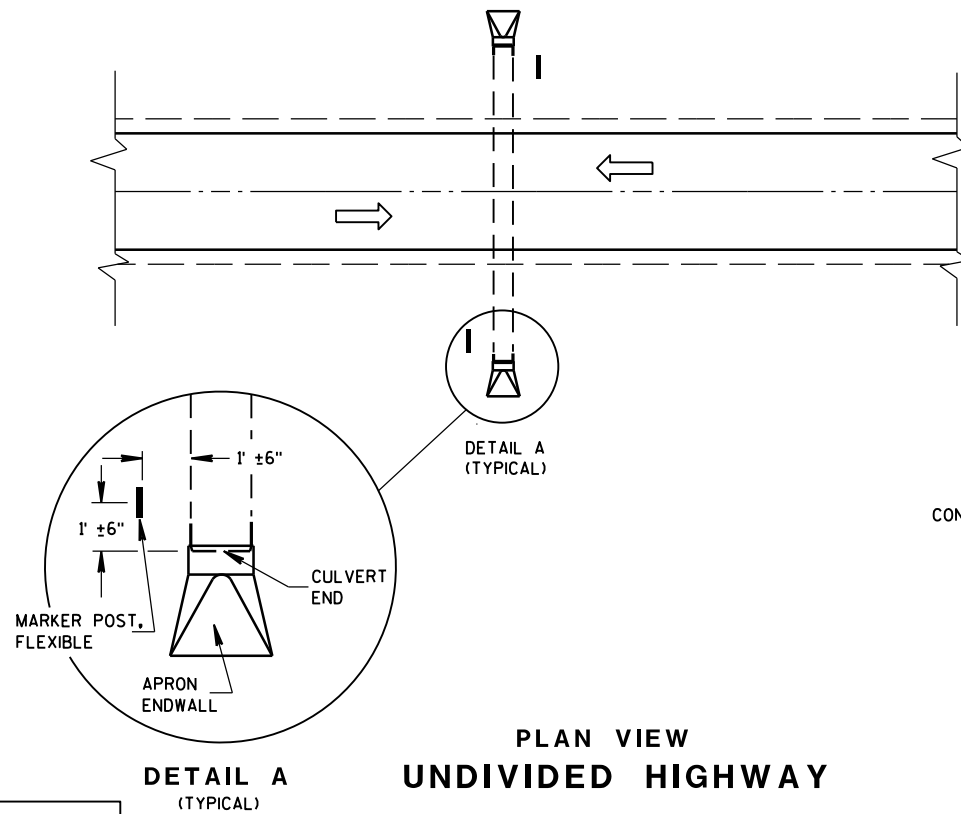
APPROVED

June, 2015 /S/ Ray Kumapayi
DATE CHIEF SURVEYING AND MAPPING ENGINEER
FHWA

PLAN VIEW
DIVIDED HIGHWAY

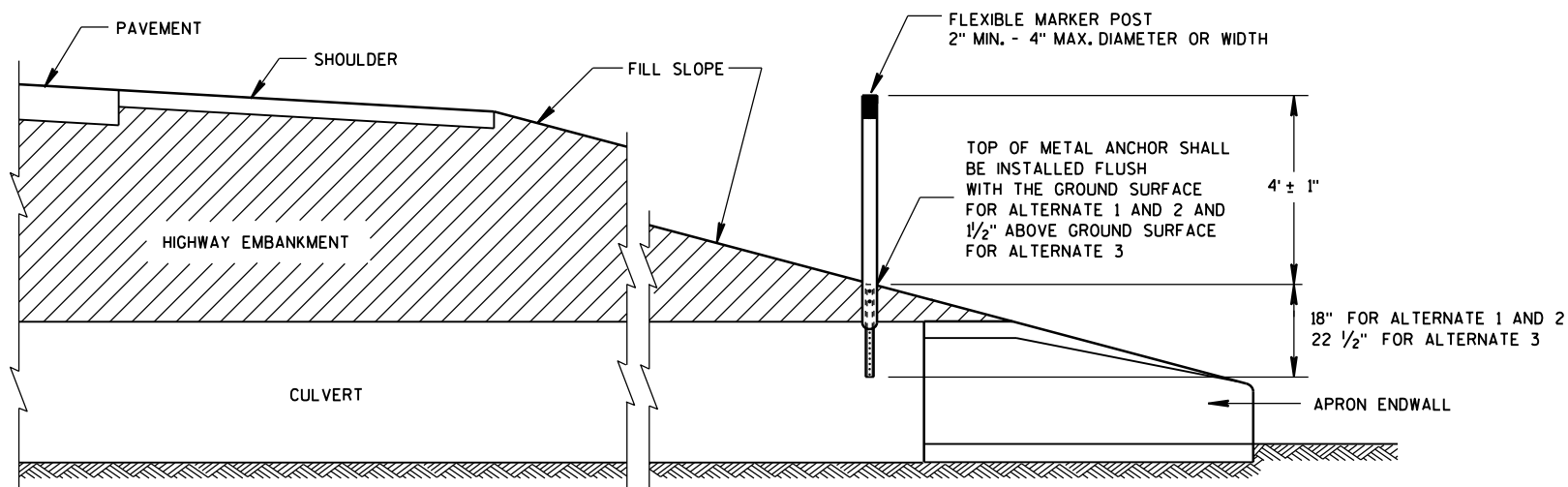


PLAN VIEW
UNDIVIDED HIGHWAY



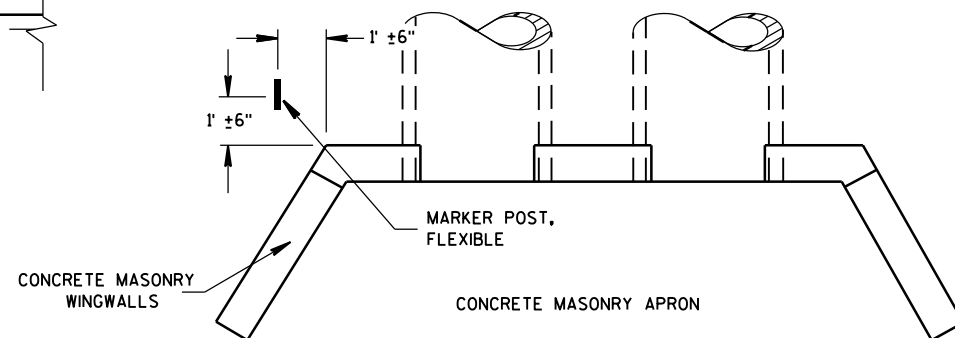
FLEXIBLE MARKER POST LOCATION

CROSS SECTION
FLEXIBLE MARKER POST



GENERAL NOTES

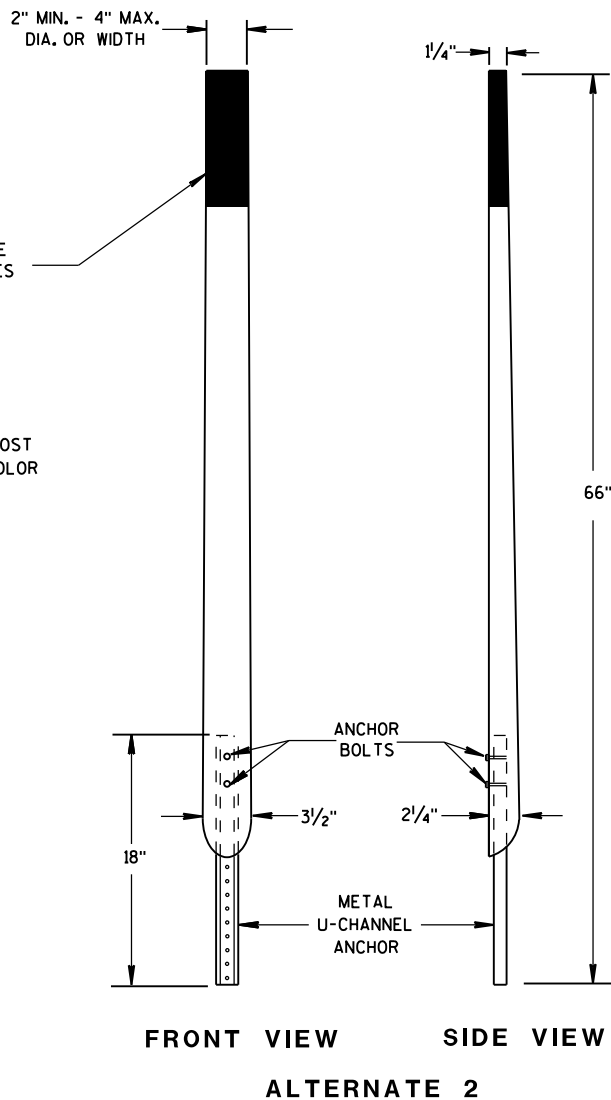
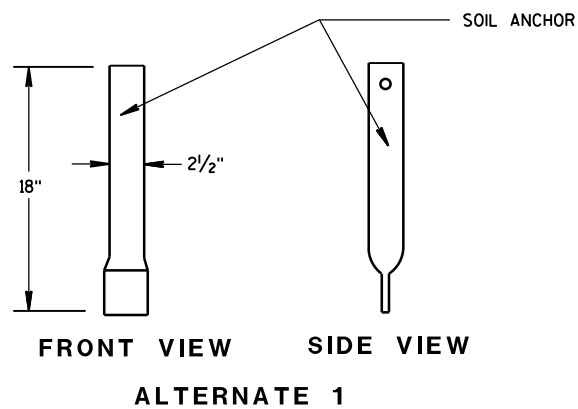
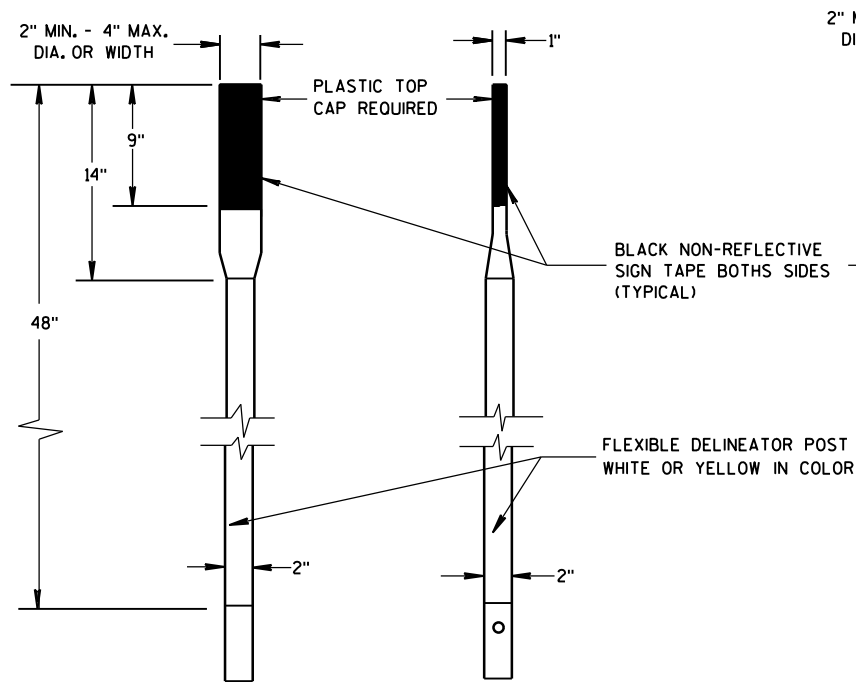
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



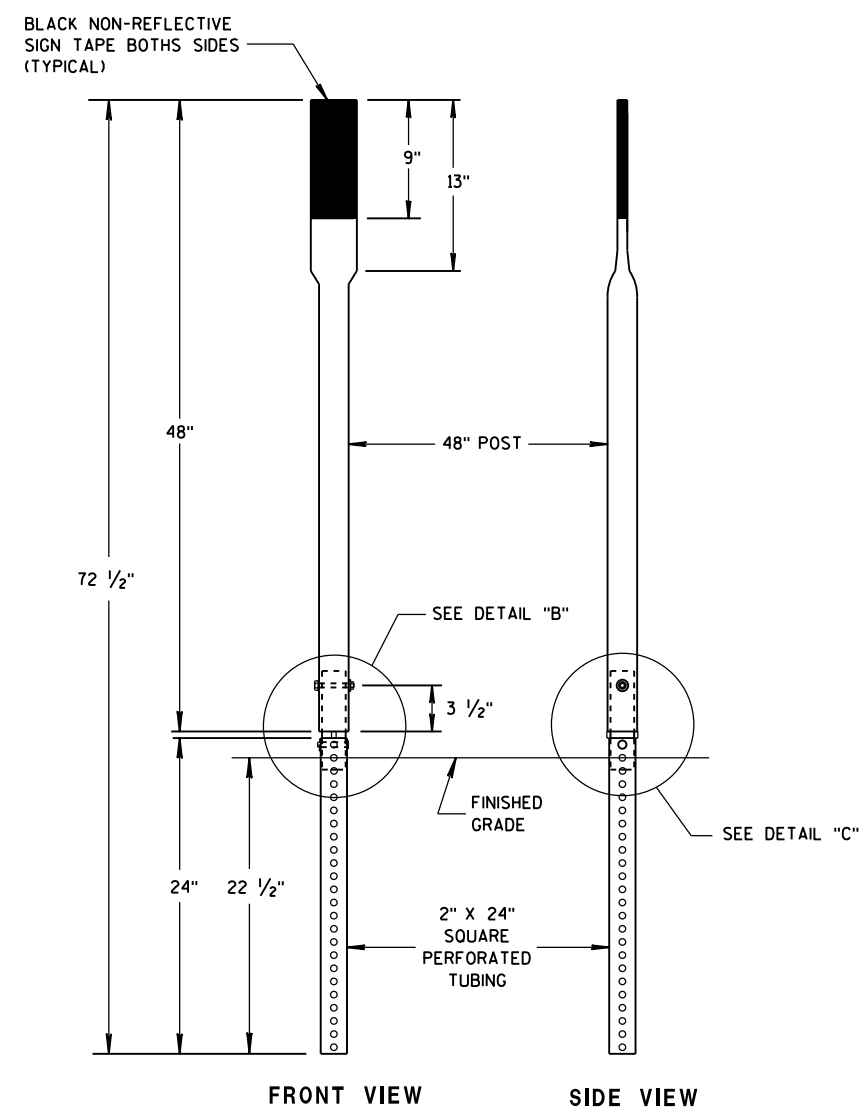
PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST
FOR CULVERT END

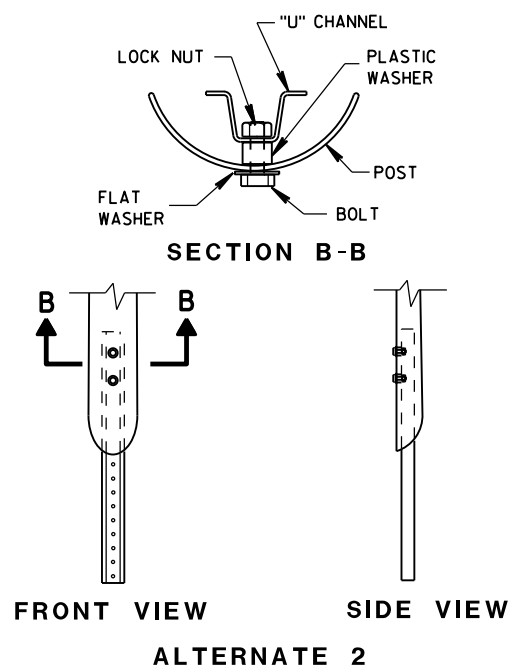
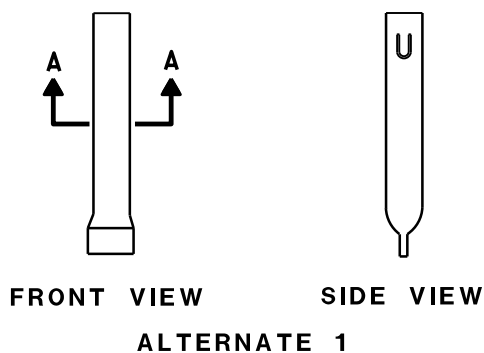
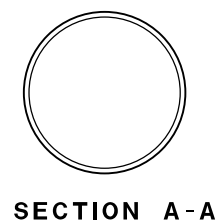
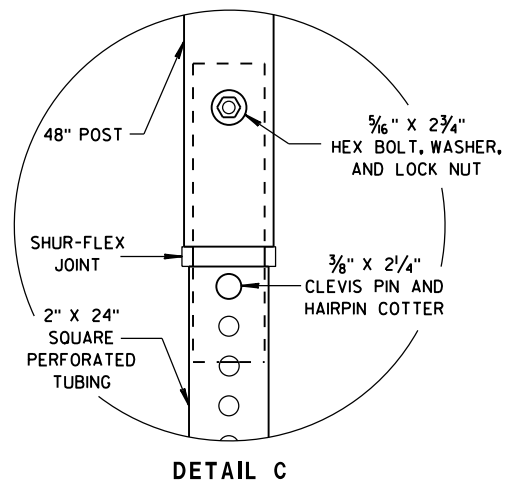
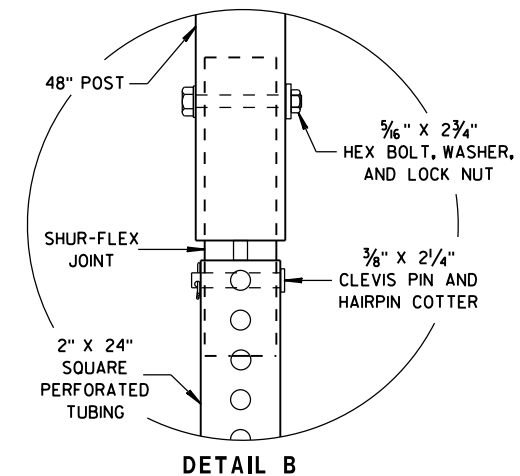
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



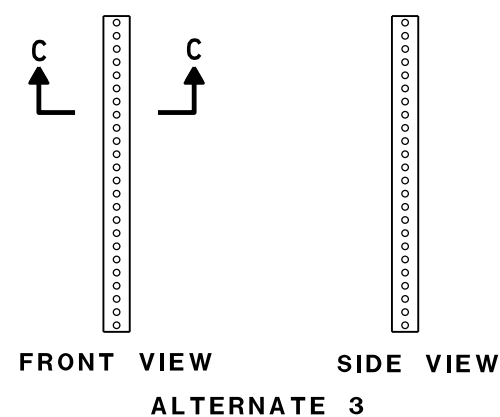
FLEXIBLE MARKER POSTS

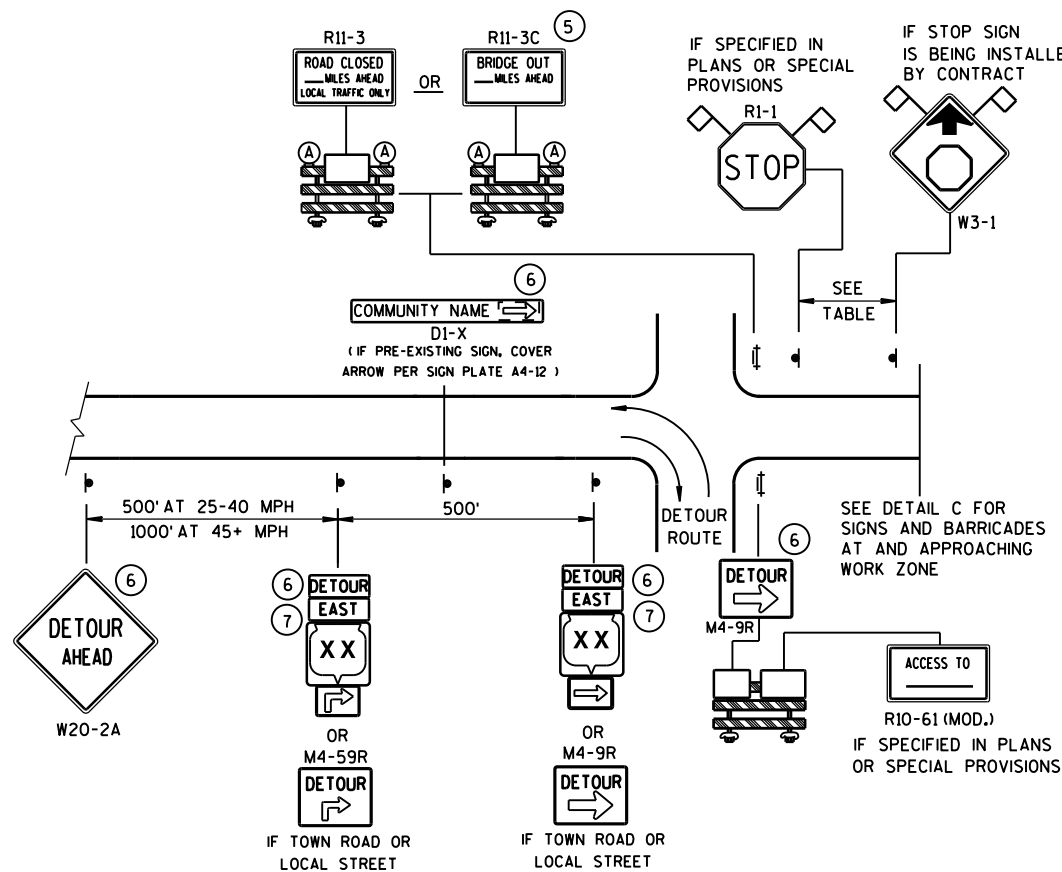


SECTION C-C

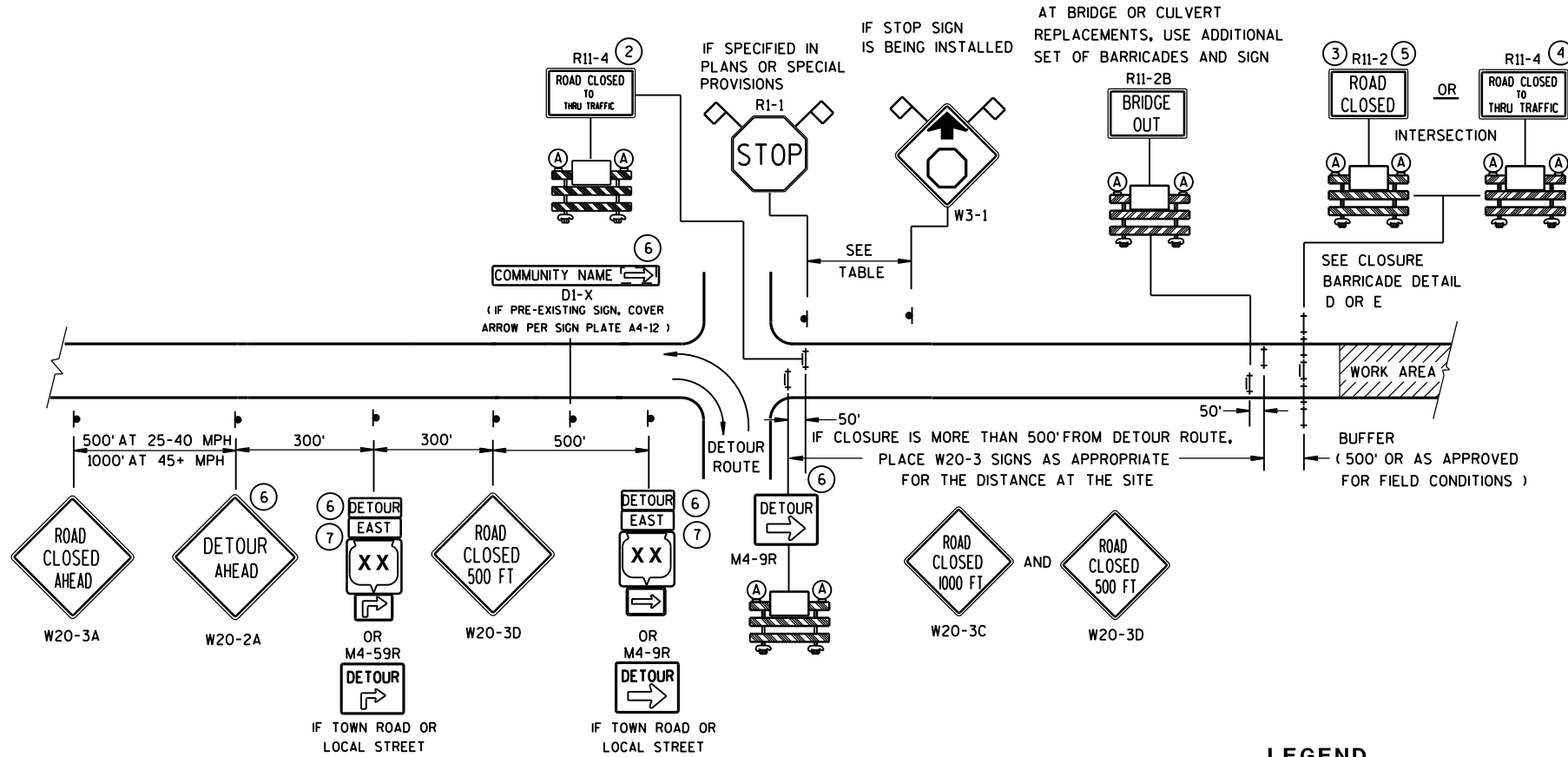


FLEXIBLE MARKER POST ANCHORS

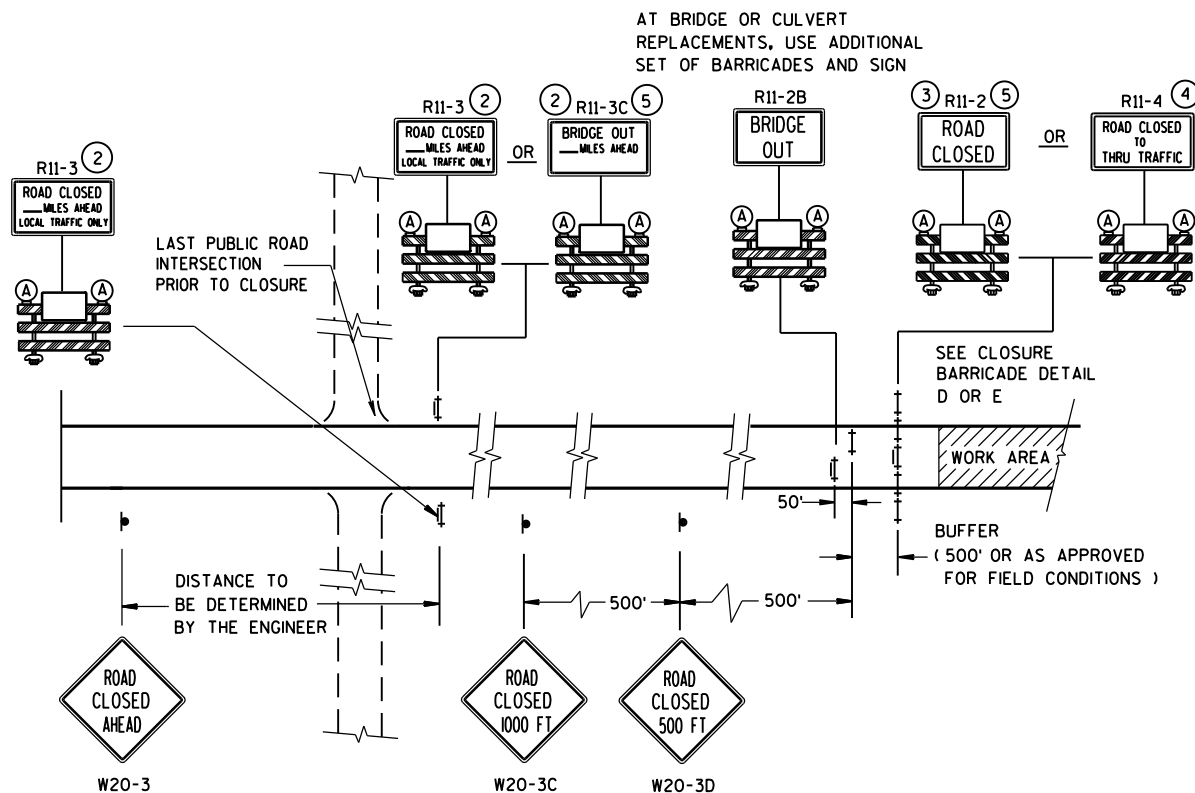
FLEXIBLE MARKER POST
FOR CULVERT ENDSTATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATIONAPPROVED
10/1/2012
DATE
FHWA/S/ Travis Feltes
STATE TRAFFIC ENGINEER OF DESIGN



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

LEGEND

- SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- Ⓐ TYPE "A" WARNING LIGHT (FLASHING)

WORK AREA

DETOUR EAST M4-8 M3-X
XX OR COUNTY XX OR XX
M1-4 M1-5A M1-6

M05-1 OR M06-1

FLAGS, 16" X 16" MIN., (ORANGE)

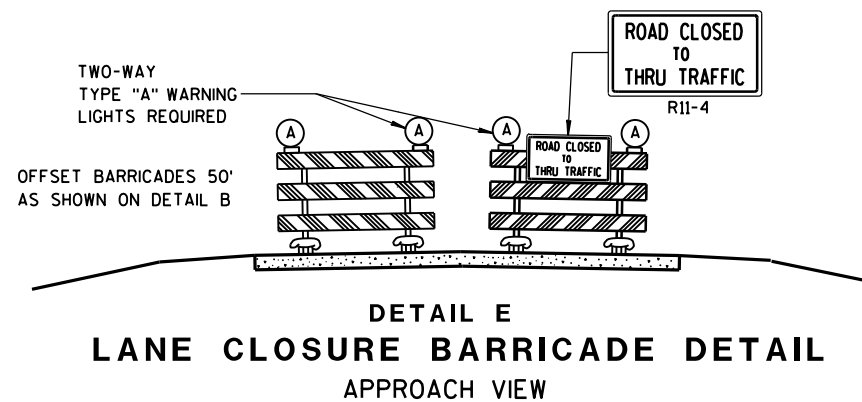
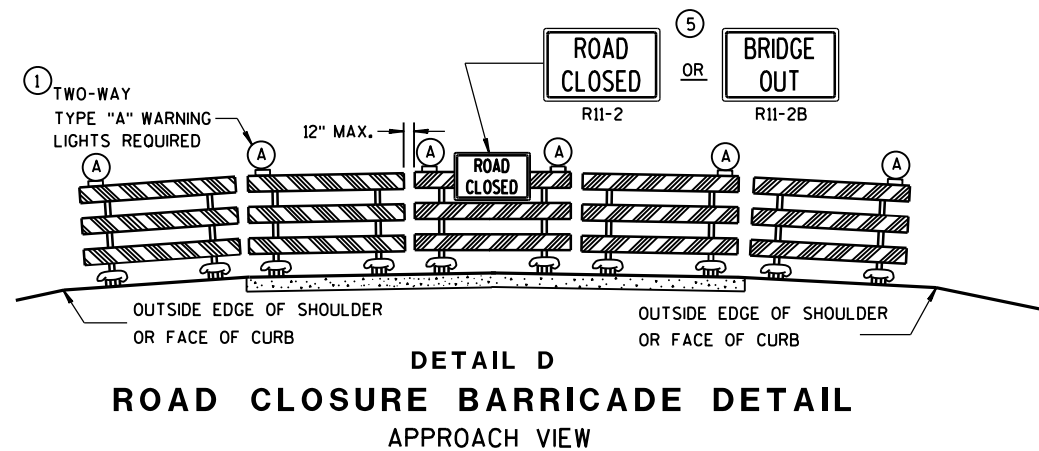
SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS
FOR
MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 /S/ Peter Amokobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".

M4-9 SHALL BE 30" X 24".

M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)

M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)

M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)

M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)

D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

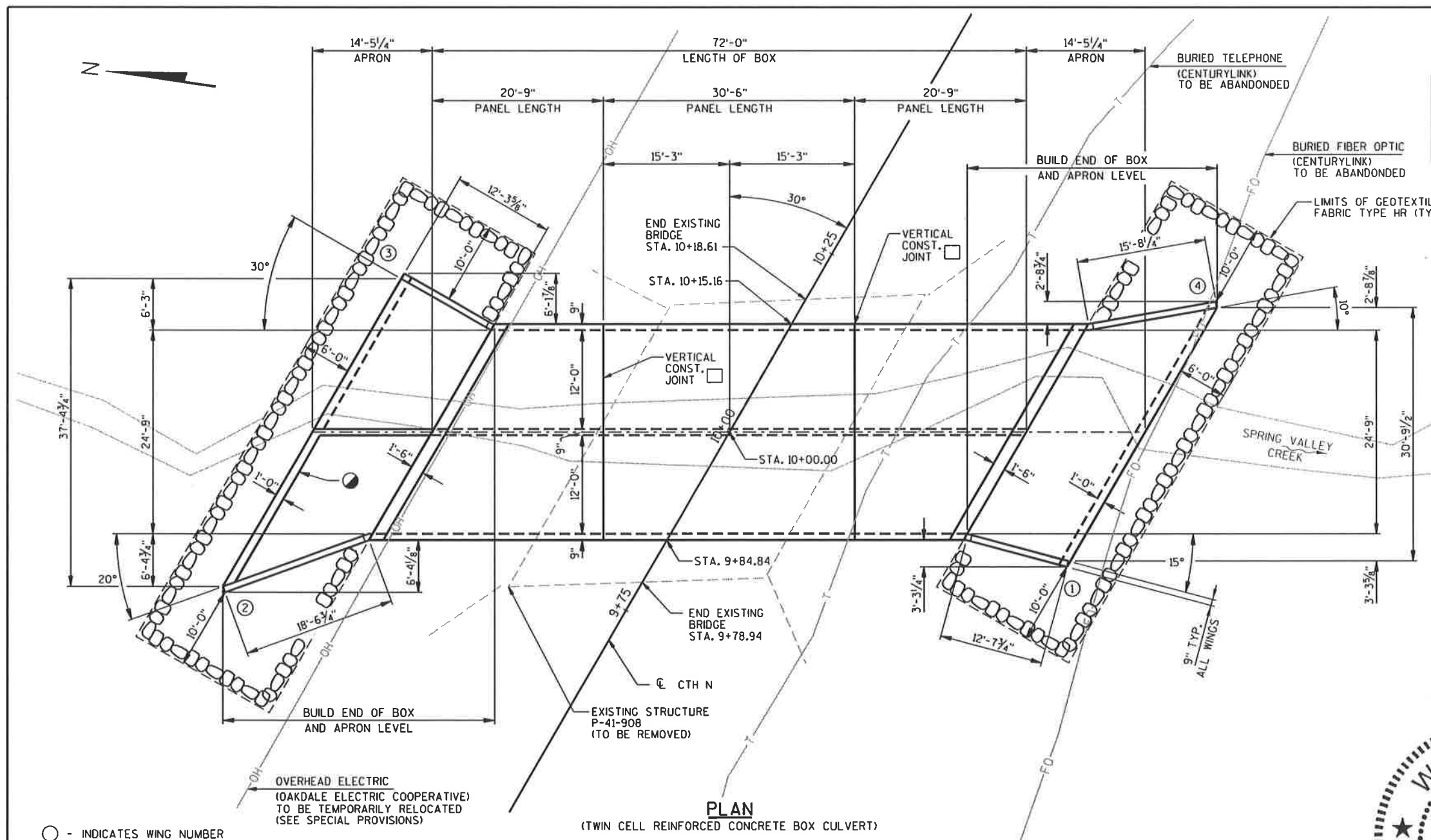
R1-1 SHALL BE 36" X 36".

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

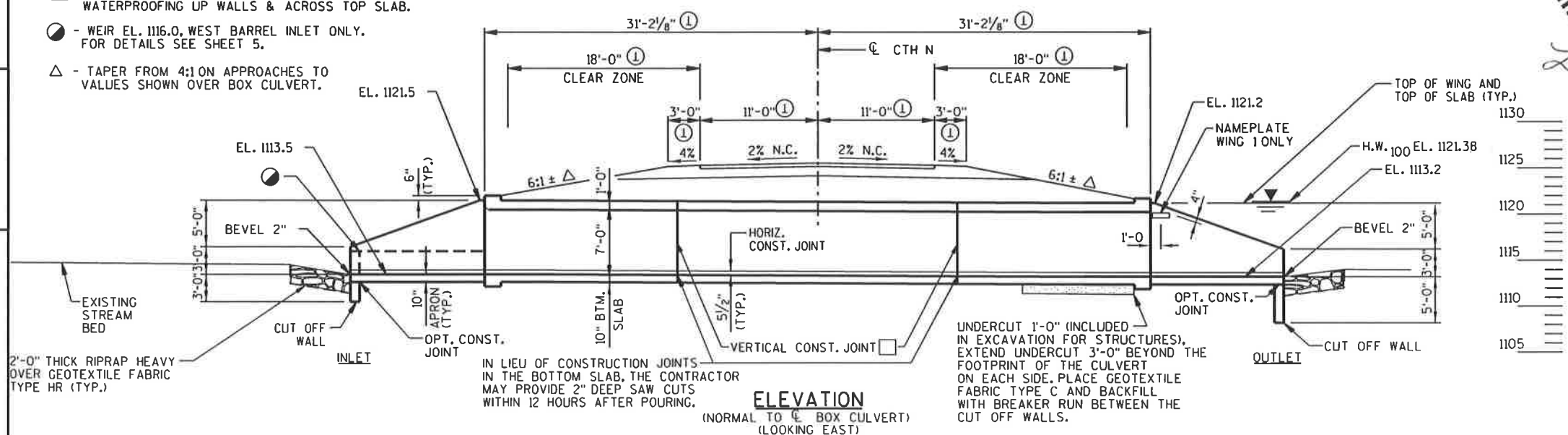
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

Sept. 2015 /S/ Peter Amokobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER



- - INDICATES WING NUMBER
- ① - DIMENSION GIVEN NORMAL TO THE CL OF CTH N.
- - 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING UP WALLS & ACROSS TOP SLAB.
- - WEIR EL. 1116.0, WEST BARREL INLET ONLY. FOR DETAILS SEE SHEET 5.
- △ - TAPER FROM 4:1 ON APPROACHES TO VALUES SHOWN OVER BOX CULVERT.



STATE PROJECT NUMBER			
5126-00-71			
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
1	10+09.23, 13.4' LT	CHIS + ON NE CORNER BRIDGE	1124.84
2	6+00.40, 21.56' RT	PK NAIL TOP OF CORRUGATED METAL PIPE DR. 24107	1132.61
3	12+19.92, 16.76' LT	PK NAIL TOP OF CORRUGATED METAL PIPE DR. 24192	1124.40

DESIGN DATA

- LIVE LOAD:**
DESIGN LOADING : HL-93
INVENTORY RATING FACTOR: 1.05
OPERATIONAL RATING FACTOR: 1.35
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV) = 255 KIPS
EARTH LOAD : DESIGNED FOR FILL HEIGHT RANGE OF 3.0 TO 4.0 FEET
- TRAFFIC DATA:**
A.A.D.T. (2017) = 200
A.A.D.T. (2037) = 240
- MATERIAL PROPERTIES:**
CONCRETE MASONRY f'c = 3,500 P.S.I.
BAR STEEL REINFORCEMENT HS STRUCTURES fy = 60,000 P.S.I.
- FOUNDATION DATA:**
ALLOWABLE SOIL BEARING CAPACITY = 4,000 P.S.F.
- HYDRAULIC DATA:**
100 YEAR FREQUENCY
DRAINAGE AREA = 2.1 SQ. MI.
Q 100 = 900 C.F.S.
VELOCITY = 5.77 F.P.S.
WATERWAY AREA = 156 SQ. FT.
HIGH WATER 100 ELEVATION = 1121.38
ROADWAY OVERFLOW DESIGN FREQUENCY > 100 YEARS
O2 HIGH WATER ELEVATION (230 C.F.S.) = 1119.09
SCOUR CRITICAL CODE = 8

LIST OF DRAWINGS

1. GENERAL PLAN
2. CROSS SECTION, QUANTITIES, NOTES & DETAILS
3. BOX CULVERT
4. BOX CULVERT DETAILS
5. INLET APRON & WING DETAILS
6. OUTLET APRON & WING DETAILS
7. SUBSURFACE EXPLORATION



NO.	DATE	REVISION	BY
MSA TRANSPORTATION • MUNICIPAL DEVELOPMENT • ENVIRONMENTAL 1220 South Boulevard Baraboo, WI 53913 608-356-2771 1-800-362-4505 Fax: 608-356-2770			
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
ACCEPTED <i>William C. Dreher</i> SDR		05/03/16 DATE	
CHIEF STRUCTURES DESIGN ENGINEER			
STRUCTURE B-41-304			
CTH N OVER SPRING VALLEY CREEK			
COUNTY	MONROE	TOWN/CITY/VILLAGE	CLIFTON
DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS			
DESIGNED BY	JAS	DESIGN CK'D.	JRS
DRAWN BY	RLR	PLANS CK'D.	LJR
GENERAL PLAN			SHEET 1 OF 7

DESIGN CONTACT:
LEAH RHODES
(608) 355-8945

BRIDGE OFFICE CONTACT:
WILLIAM DREHER
(608) 266-8489

STRUCTURE ESTIMATED QUANTITIES

ITEM NUMBER	BID ITEM	UNIT	TOTAL
203.0600.S.01	REMOVING OLD STRUCTURE OVER WATERWAY WITH MINIMAL DEBRIS STATION 10+00	LS	1
206.2000.01	EXCAVATION FOR STRUCTURES CULVERTS B-41-304	LS	1
210.0100	BACKFILL STRUCTURE	CY	1055
311.0115	BREAKER RUN	CY	125
504.0100	CONCRETE MASONRY CULVERTS	CY	223
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	32,170
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	30
606.0300	RIPRAP HEAVY	CY	110
645.0105	GEOTEXTILE FABRIC TYPE C	SY	400
645.0120	GEOTEXTILE FABRIC TYPE HR	SY	250
SPV.0105.04	TEMPORARY WATER DIVERSION, CULVERT B-41-304	LS	1
	NON-BID ITEMS		
	PREFORMED FILLER	SIZE	¾"

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.

THE FIRST DIGIT OF A THREE DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

THE CONCRETE IN THE CUT OFF WALL MAY BE PLACED UNDERWATER IF THE EXCAVATION CANNOT BE DEWATERED.

THE ALTERNATE CUT OFF WALL MAY BE USED IN LIEU OF THE CAST IN PLACE CUT OFF WALLS.
PAYMENT SHALL BE BASED ON CONCRETE CUT OFF WALLS.

PEAT WAS NOT FOUND IN THE SOIL BORINGS AND IS NOT ANTICIPATED. IF PEAT IS FOUND BELOW THE BOX CULVERT DURING EXCAVATION, REMOVE THE PEAT AND REPLACE IT WITH COMPACTED BREAKER RUN.

ALL SPACES EXCAVATED AND NOT OCCUPIED BY THE NEW STRUCTURE AND BREAKER RUN SHALL BE BACKFILLED WITH BACKFILL STRUCTURE TO THE ELEVATION OF THE ROADWAY SUBGRADE. BACKFILL STRUCTURE IS REQUIRED BEHIND ALL WINGWALLS. THE BACKFILL STRUCTURE ESTIMATED QUANTITIES ASSUMED A 1½:1 EXCAVATION SLOPE.

THIS STRUCTURE WILL REPLACE EXISTING BRIDGE P-41-908, A 39.4 FT. LONG SINGLE SPAN STEEL DECK GIRDER BRIDGE ON FULL RETAINING CONCRETE ABUTMENTS. SALVAGE EXISTING INTERIOR BEAMS AS NOTED IN THE SPECIAL PROVISIONS.

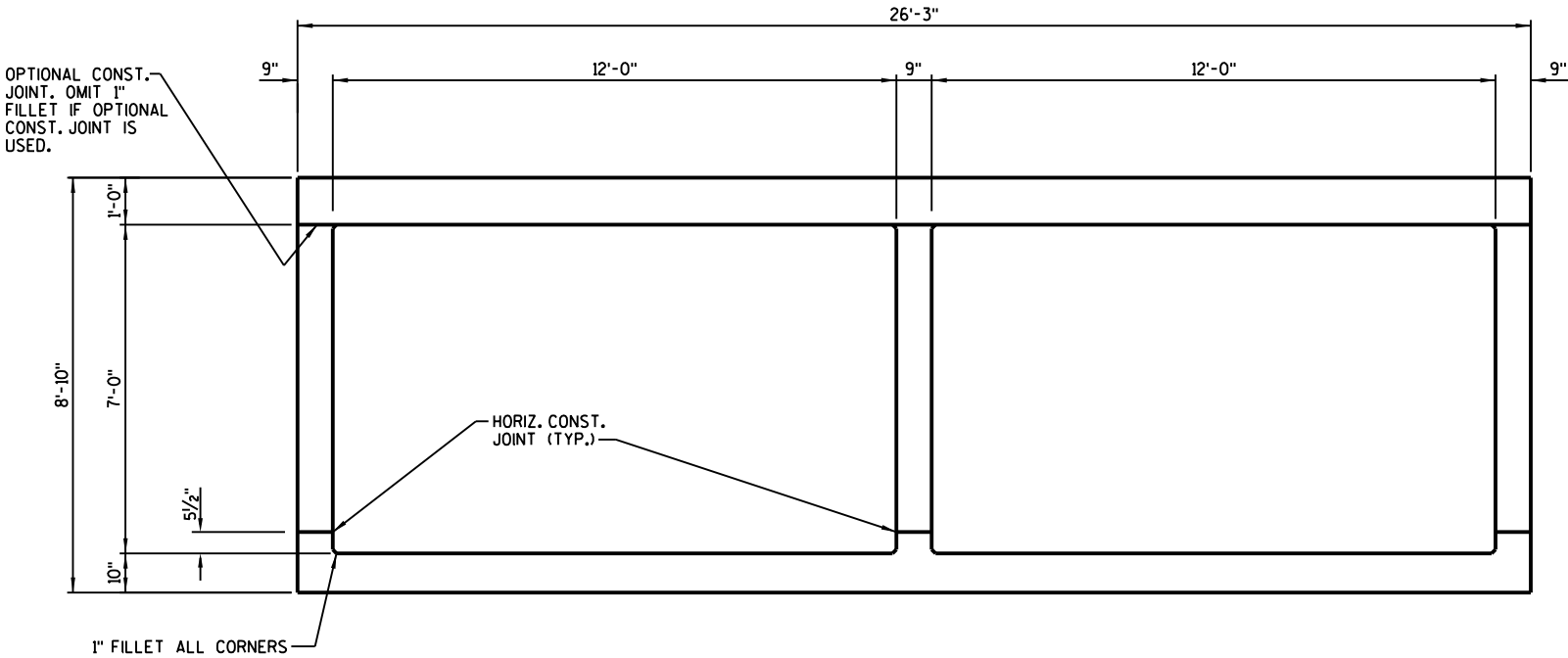
THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES CULVERTS B-41-304" SHALL BE THE EXISTING GROUND LINE.

THE CONTRACTOR MAY FURNISH A PRECAST CONCRETE BOX CULVERT IN LIEU OF THE CAST-IN-PLACE BOX CULVERT WITH THE ACCEPTANCE OF THE SHOP DRAWINGS BY THE STRUCTURES DESIGN SECTION. THE PRECAST CONCRETE BOX CULVERT SHALL CONFORM TO PRECAST DETAILS IN CHAPTER 36 STANDARDS OF THE CURRENT WISCONSIN DOT BRIDGE MANUAL. PAYMENT FOR THE PRECAST CULVERT SHALL BE BASED ON THE QUANTITIES AND PRICES BID FOR THE ITEMS LISTED IN THE "STRUCTURE ESTIMATED QUANTITIES".

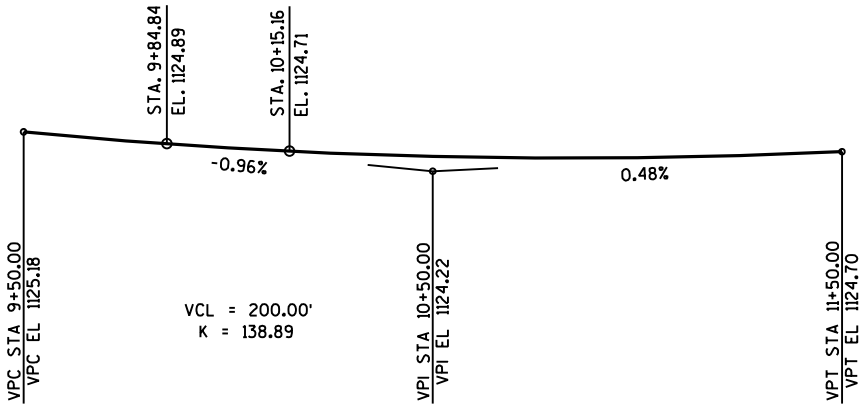
IN LIEU OF USING BREAKER RUN FOR THE BOX CONSTRUCTION PLATFORM, THE CONTRACTOR MAY ELECT TO SUBSTITUTE #1 OR #2 CONCRETE COARSE AGGREGATE, SELECT CRUSHED MATERIAL OR OTHER GRANULAR MATERIAL AS APPROVED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR BASE STABILITY WITH ANY SUBSTITUTED MATERIAL. THE REGION GEOTECHNICAL ENGINEER MAY BE CONTACTED TO DETERMINE IF "OTHER GRANULAR MATERIAL" IS ACCEPTABLE.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO NAVD 88 (1996 ADJUSTED), AND WERE ESTABLISHED AT THE SITE USING GPS TECHNOLOGY.

APRONS AND BOTTOM SLAB MAY BE POURED CONTINUOUSLY.



TYPICAL SECTION THRU BOX



PROFILE GRADE LINE - CTH N

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-41-304			
DRAWN BY RLR		PLANS CK'D. LJR	
CROSS SECTION, QUANTITIES, NOTES & DETAILS			SHEET 2 OF 7



NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE		B-41-304	
DRAWN BY		RLR	PLANS CK'D. JAS
BOX CULVERT		SHEET 3 OF 7	

BILL OF BARS

UNCOATED 28,245 LBS.

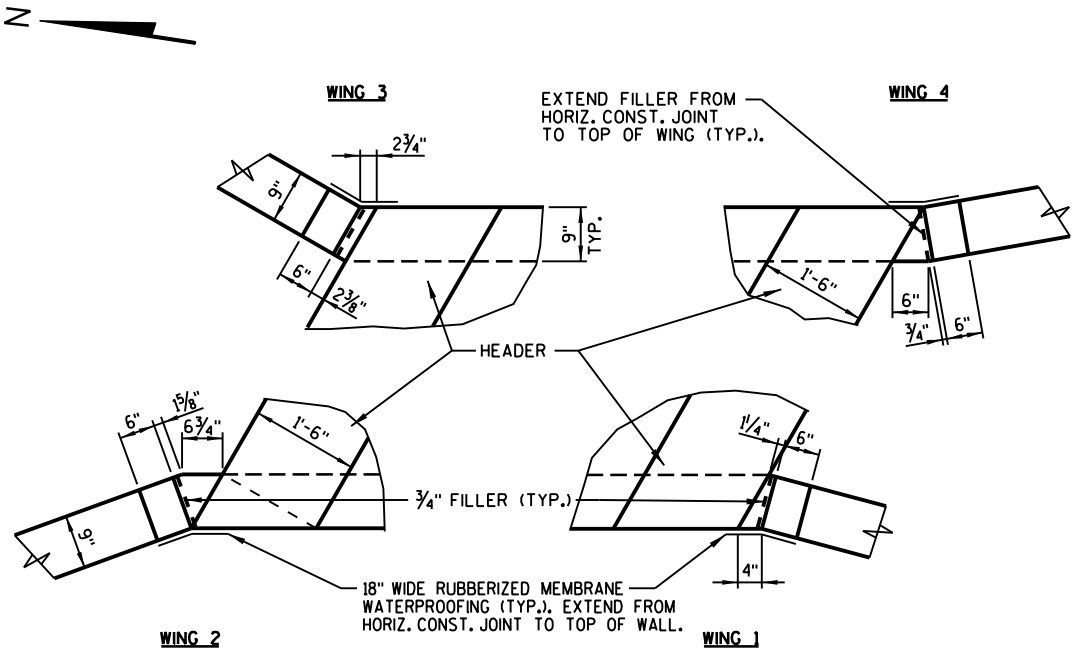
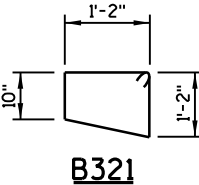
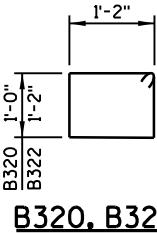
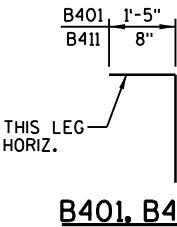
MARK	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
B401	572	6'-3"	X		BOX CORNERS - VERT.
B602	113	25'-11"			BOTTOM SLAB - EXTERIOR - TRANS.
B603	56	13'-11"		*	END - BOTTOM SLAB - EXTERIOR - TRANS.
B404	101	30'-2"			CENTER PANEL ALL WALLS & SLABS - LONGIT.
B405	96	20'-4"		*	END - BOTTOM & TOP OF TOP - LONGIT.
B506	77	25'-11"			TOP OF BOTTOM SLAB - TRANS.
B507	36	13'-9"		*	END - TOP OF BOTTOM SLAB - TRANS.
B408	98	2'-2"			WALL DOWELS - BOTTOM - VERT.
B409	98	7'-4"			WALLS - TOP - VERT.
B410	194	2'-6"			PIER DOWELS - BOTTOM - VERT.
B411	194	7'-11"	X		PIER - TOP - VERT.
B412	24	27'-11"			LONG EXTERIOR WALL - LONGIT.
B413	24	13'-0"			SHORT EXTERIOR WALL - LONGIT.
B414	16	20'-7"			PIER - LONGIT.
B615	77	25'-11"			BOTTOM OF TOP SLAB - TRANS.
B616	36	13'-9"		*	END - BOTTOM OF TOP SLAB - TRANS.
B417	42	20'-4"		*	END - BOTTOM OF TOP SLAB - LONGIT.
B518	113	25'-11"			TOP SLAB - EXTERIOR - TRANS.
B519	56	13'-11"		*	END - TOP SLAB - EXTERIOR - TRANS.
B320	70	4'-9"	X		BOTTOM HEADERS - STIRRUP - VERT.
B321	35	4'-9"	X		INLET TOP HEADER - STIRRUP - VERT.
B322	35	5'-1"	X		OUTLET TOP HEADER - STIRRUP - VERT.
B923	24	29'-11"			HEADERS - TRANS.
B524	150	4'-0"			CONSTRUCTION JOINT DOWEL - LONGIT.

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.

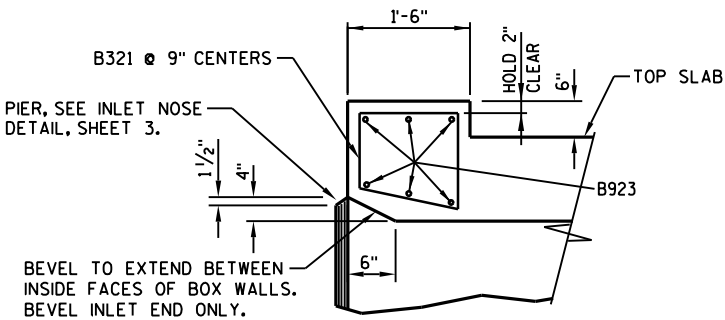
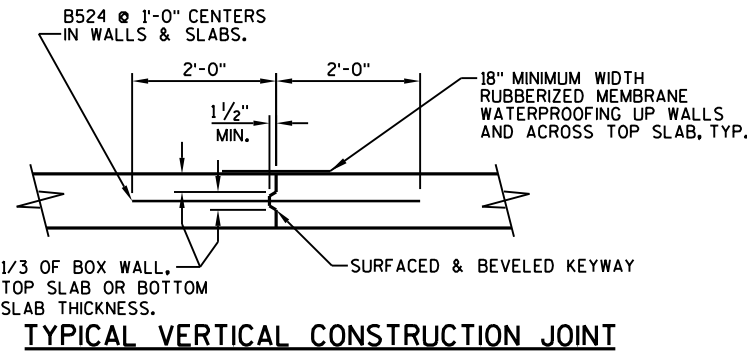
* - LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

BAR MARK	NO. REQ'D.	LENGTH
B603	2 SERIES OF 28	2'-3" TO 25'-7"
B405	6 SERIES OF 16	13'-10" TO 26'-10"
B507	2 SERIES OF 18	2'-8" TO 24'-9"
B616	2 SERIES OF 18	2'-8" TO 24'-9"
B417	2 SERIES OF 21	13'-7" TO 27'-1"
B519	2 SERIES OF 28	2'-3" TO 25'-7"

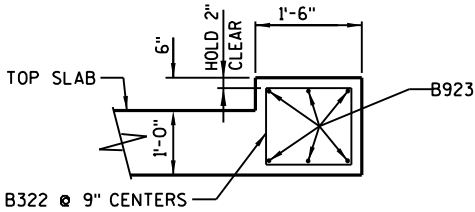
BAR SERIES TABLE
BUNDLE AND TAG EACH SERIES SEPARATELY



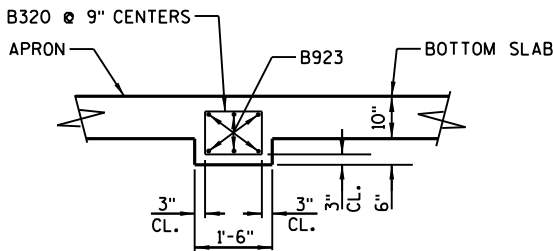
CORNER DETAILS
SEE SHEETS 5 & 6 FOR WING DETAILS



SECTION THRU INLET TOP HEADER



SECTION THRU OUTLET TOP HEADER



SECTION THRU BOTTOM HEADERS

NO.	DATE	REVISION	BY
		STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
		STRUCTURE B-41-304	
		DRAWN BY RLR	PLANS CK'D. LJR
		BOX CULVERT DETAILS	SHEET 4 OF 7

LEGEND

○ — INDICATES WING NUMBER

F.F. — FRONT FACE

B.F. — BACK FACE

CL. — CLEAR

BILL OF BARS

UNCOATED 2135 LBS.

MARK	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
A401	38	3'-6"	X		INLET APRON CUT-OFF WALL - VERT.
A402	3	43'-8"			INLET APRON CUT-OFF WALL - TRANS.
A403	14	36'-9"	*		INLET APRON - TRANS.
A404	26	16'-6"			INLET APRON - LONGIT.
A405	5	8'-7"	*		INLET APRON @ WING 2 - LONGIT.
A406	5	8'-2"	*		INLET APRON @ WING 3 - LONGIT.
A407	14	2'-6"			INLET WINGS - F.F. DOWELS - VERT.
A508	48	9'-2"	X		INLET WINGS - B.F. - VERT.
A409	1	7'-11"	X		INLET WING 3 - B.F. - END - VERT.
A410	1	7'-6"	X		INLET WING 3 - B.F. - END - VERT.
A411	1	7'-1"	X		INLET WING 3 - B.F. - END - VERT.
A412	4	4'-5"	*		INLET WING 3 - F.F. - END - VERT.
A413	5	6'-2"	*		INLET WING 3 - F.F. - VERT.
A414	20	5'-5"	*		INLET WING 3 - B.F. - VERT.
A415	6	11'-11"			INLET WING 3 & APRON - LONGIT.
A416	2	9'-4"			INLET WING 3 - F.F. & B.F. - LONGIT.
A417	2	5'-9"			INLET WING 3 - F.F. & B.F. - LONGIT.
A418	2	2'-3"			INLET WING 3 - F.F. & B.F. - LONGIT.
A519	2	12'-11"			INLET WING 3 - TOP - LONGIT.
A420	4	4'-2"	*		INLET WING 2 - F.F. - END - VERT.
A421	5	7'-8"	X	*	INLET WING 2 - B.F. - END - VERT.
A422	28	5'-6"	*		INLET WING 2 - B.F. - VERT.
A423	9	5'-9"	*		INLET WING 2 - F.F. - VERT.
A424	6	18'-2"			INLET WING 2 & APRON - LONGIT.
A425	2	14'-1"			INLET WING 2 - F.F. & B.F. - LONGIT.
A426	2	8'-8"			INLET WING 2 - F.F. & B.F. - LONGIT.
A427	2	3'-3"			INLET WING 2 - F.F. & B.F. - LONGIT.
A528	2	18'-10"			INLET WING 2 - TOP - LONGIT.
A429	71	3'-3"	X		INLET APRON WEIR - VERT.
A430	4	21'-0"			INLET APRON WEIR - TRANS.
A431	2	15'-4"	X		INLET APRON WEIR - LONGIT.
A432	2	15'-8"	X		INLET APRON WEIR - LONGIT.

BAR SERIES TABLE

MARK	NO. REQUIRED	LENGTH
A403	1 SERIES OF 14	30'-0" TO 43'-5"
A405	1 SERIES OF 5	4'-3" TO 12'-11"
A406	1 SERIES OF 5	3'-6" TO 12'-9"
A412	1 SERIES OF 4	3'-6" TO 5'-4"
A413	1 SERIES OF 5	4'-11" TO 7'-4"
A414	1 SERIES OF 20	3'-5" TO 7'-4"
A420	1 SERIES OF 4	3'-6" TO 4'-9"
A421	1 SERIES OF 5	7'-1" TO 8'-2"
A422	1 SERIES OF 28	3'-8" TO 7'-4"
A423	1 SERIES OF 9	4'-1" TO 7'-4"

BUNDLE AND TAG EACH SERIES SEPARATELY

MARK	A
A401	1'-0"
A508	6'-9"
A409	
A410	3'-8"
A411	
A421	
A429	8"

A431, A432

THIS LEG
HORIZ.

SECTION B-B THRU WEIR

SECTION A-A THRU WEIR

INLET APRON PLAN

SECTION THRU INLET CUT-OFF WALL

SEE SHEET 6 FOR ALTERNATE CUT-OFF WALL DETAIL

WING 3

INLET WING ELEVATIONS

WING 2

DIMENSIONS IN BENDING DETAILS ARE OUT TO OUT OF BAR.

* - LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS. BENT BARS IF USED IN BAR SERIES TABLE SHALL BE BENT AFTER CUTTING.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-41-304			
DRAWN BY RLR		PLANS CK'D. JAS	
INLET APRON & WING DETAILS			SHEET 5 OF 7

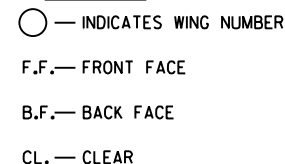
UNCOATED 1790 LBS.

* - LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS. BENT BARS IF USED IN BAR SERIES TABLE SHALL BE BENT AFTER CUTTING.

MARK	NO. REQUIRED	LENGTH
C403	1 SERIES OF 14	29'-11" TO 36'-4"
C414	1 SERIES OF 20	3'-5" TO 7'-4"
C415	1 SERIES OF 4	3'-6" TO 5'-4"
C416	1 SERIES OF 5	4'-11" TO 7'-4"
C422	1 SERIES OF 4	3'-6" TO 5'-0"
C423	1 SERIES OF 4	7'-1" TO 8'-1"
C424	1 SERIES OF 24	3'-7" TO 7'-4"
C425	1 SERIES OF 7	4'-5" TO 7'-4"

MARK	A
C401	1'-0"
C510	6'-9"
C411	3'-8"
C412	
C413	
C423	

FILE= 5848017_06.dgn
DATE= 2/17/2016





CTH N

BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	6-02-2015	336,531.4	736,352.4
2	6-02-2015	336,500.1	736,399.2
BORINGS COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
REPORT COMPLETED BY: NUMMELIN TESTING SERVICES, INC.			
ALL COORDINATES REFERENCED TO WCCS NAD 83(2011) MONROE COUNTY			

BORING #2

STA. 10+15.16

10+25

STA. 10+00.00

STA. 9+84.84

BORING #1

EXISTING STRUCTURE
P-41-908
(TO BE REMOVED)

SPRING VALLEY
CREEK

BORING #1, STA. 9+73
EL. 1124.9, 6' LT.

BORING #2, STA. 10+27
EL. 1124.6, 6' RT.

CTH N

EL. 1121.5

EL. 1121.2

EL. 1113.5

EL. 1113.2

(PROBABLE WEATHERED SANDSTONE)

50/5" SPOON REFUSAL

50/2" SPOON REFUSAL

(PROBABLE WEATHERED SANDSTONE)

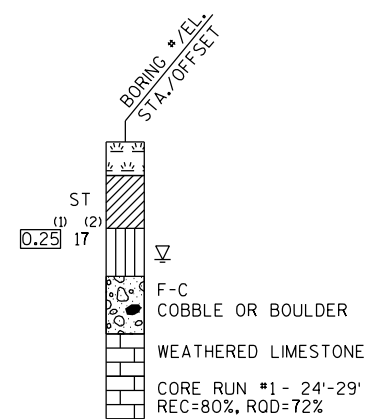
STATE PROJECT NUMBER

5126-00-71

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

▽ AT TIME OF DRILLING
▽ END OF DRILLING
▽ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

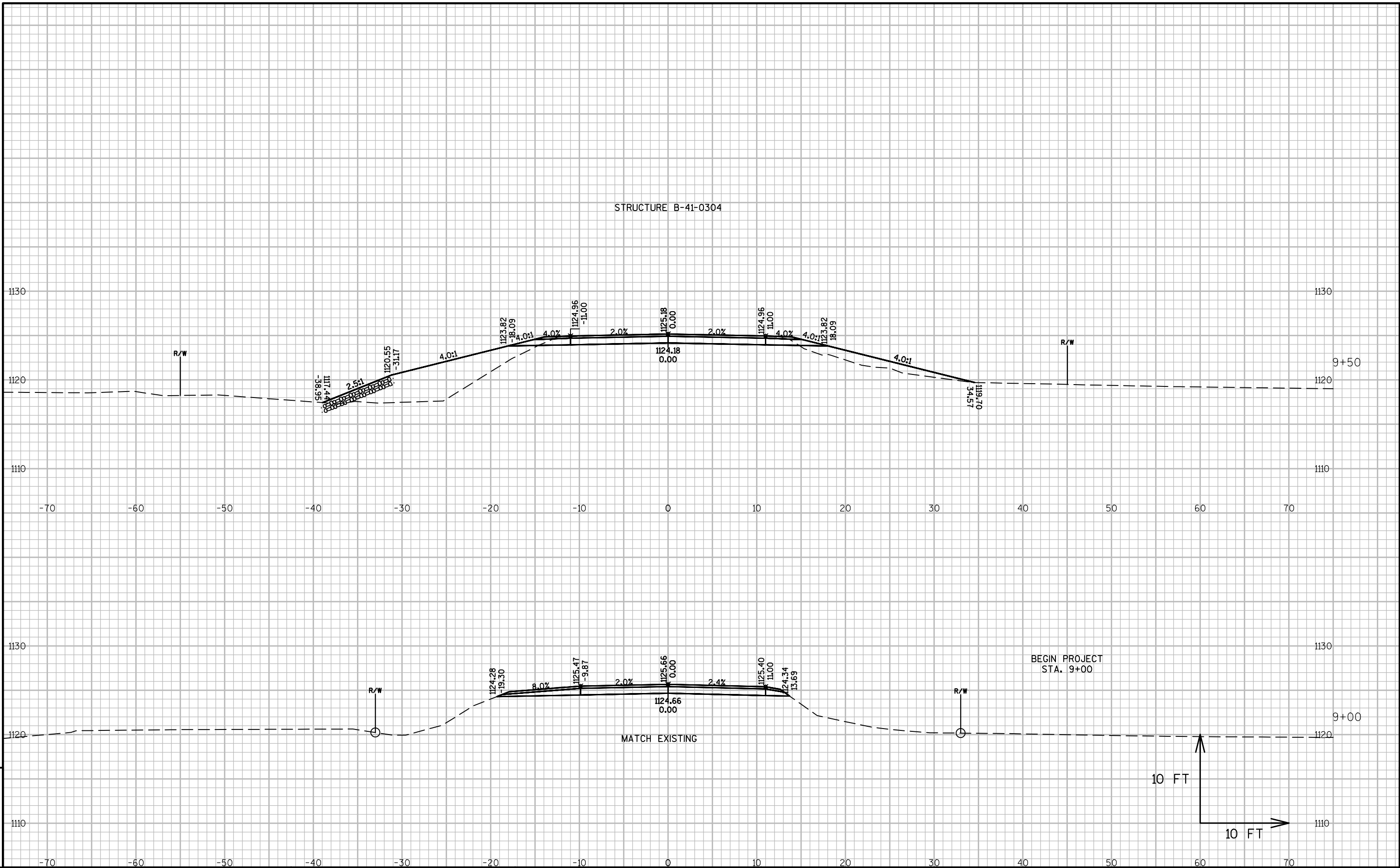
SUBSURFACE EXPLORATION FOR FOUNDATION
DESIGN AND BIDDERS INFORMATION

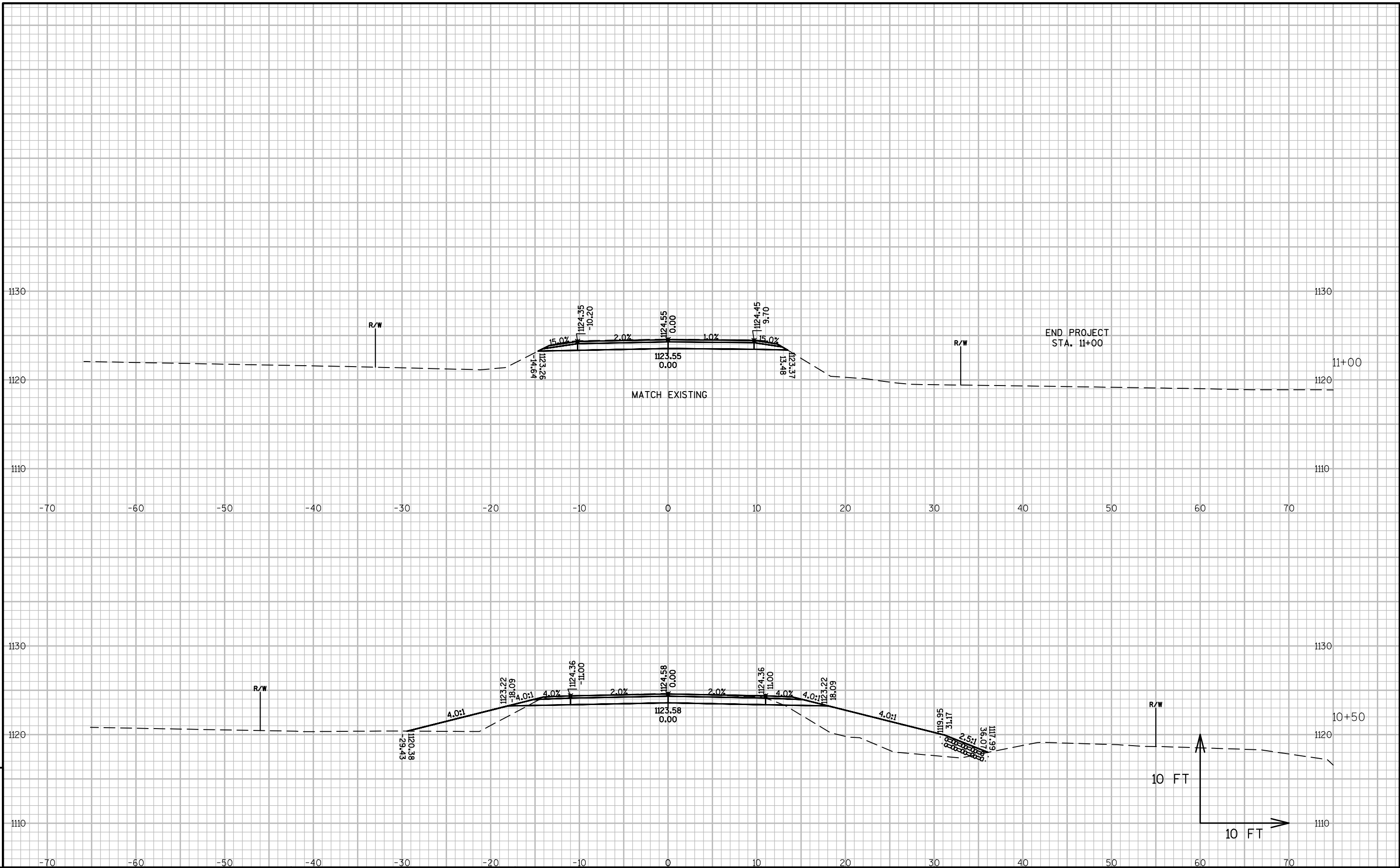
BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-41-304			
DRAWN BY		RLR	PLANS CK'D. JAS
SUBSURFACE EXPLORATION		SHEET 7 OF 7	

PROJECT I.D. 5126-00-71 EARTHWORK SUMMARY

STA	EXCAVATION (3) COMMON CY	EXCAVATION ROCK CY	FILL (1) CY	EXPANDED FILL (2) CY	WASTE CY	BORROW CY
9+00.00	52	0	68	88	-36	36
9+50.00	13	0	35	46	-33	33
9+63.00	STRUCTURE B-41-0304					
10+37.00	13	0	32	42	-29	29
10+50.00	49	0	62	81	-32	32
11+00.00	SUBTOTALS					
WEST APPROACH	65	0	103	134	-69	69
EAST APPROACH	62	0	94	123	-61	61
UNUSABLE PAVEMENT (4)						58
TOTALS	127	0	197	257	-130	188
(1) - NOT A BID ITEM - FOR INFORMATIONAL PURPOSES ONLY. (2) - FILL EXPANSION 30% (3) - EXISTING UNUSABLE ASPHALTIC PAVEMENT IS INCLUDED IN COMMON EXCAVATION TOTALS. (4) - EXISTING UNUSABLE PAVEMENT BASED ON AVERAGE THICKNESS OF 5.5" OF ASPHALT PER BORING LOG.						







Wisconsin Department of Transportation

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through innovation and exceptional service.

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