

LAX

JUN 2016

WITH:

PROJECT ID: 1000-20-80

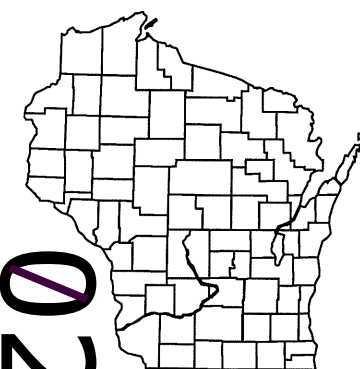
COUNTY:

STATEWIDE

ORDER OF SHEETS

- Section No. 1 Title
Section No. 2 Typical Sections and Details
Section No. 3 Estimate of Quantities
Section No. 3 Miscellaneous Quantities
~~Section No. 4 Right of Way Plat~~
~~Section No. 5 Plan and Profile~~
Section No. 6 Standard Detail Drawings
Section No. 7 Sign Plates
~~Section No. 8 Structure Plans~~
~~Section No. 9 Computer Earthwork Data~~
~~Section No. 9 Cross Sections~~

TOTAL SHEETS = 86



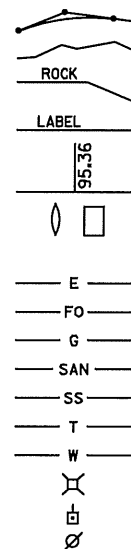
DESIGN DESIGNATION

- A.A.D.T. =
A.A.D.T. =
D.H.V. =
D.D. =
T. =
DESIGN SPEED =
ESALS =

CONVENTIONAL SYMBOLS

- PLAN
CORPORATE LIMITS
PROPERTY LINE
LOT LINE
LIMITED HIGHWAY EASEMENT
EXISTING RIGHT OF WAY
PROPOSED OR NEW R/W LINE
SLOPE INTERCEPT
REFERENCE LINE
EXISTING CULVERT
PROPOSED CULVERT (Box or Pipe)
COMBUSTIBLE FLUIDS
MARSH AREA
WOODED OR SHRUB AREA

- PROFILE
GRADE LINE
ORIGINAL GROUND
MARSH OR ROCK PROFILE (To be noted as such)
SPECIAL DITCH
GRADE ELEVATION
CULVERT (Profile View)
UTILITIES
ELECTRIC
FIBER OPTIC
GAS
SANITARY SEWER
STORM SEWER
TELEPHONE
WATER
UTILITY PEDESTAL
POWER POLE
TELEPHONE POLE



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

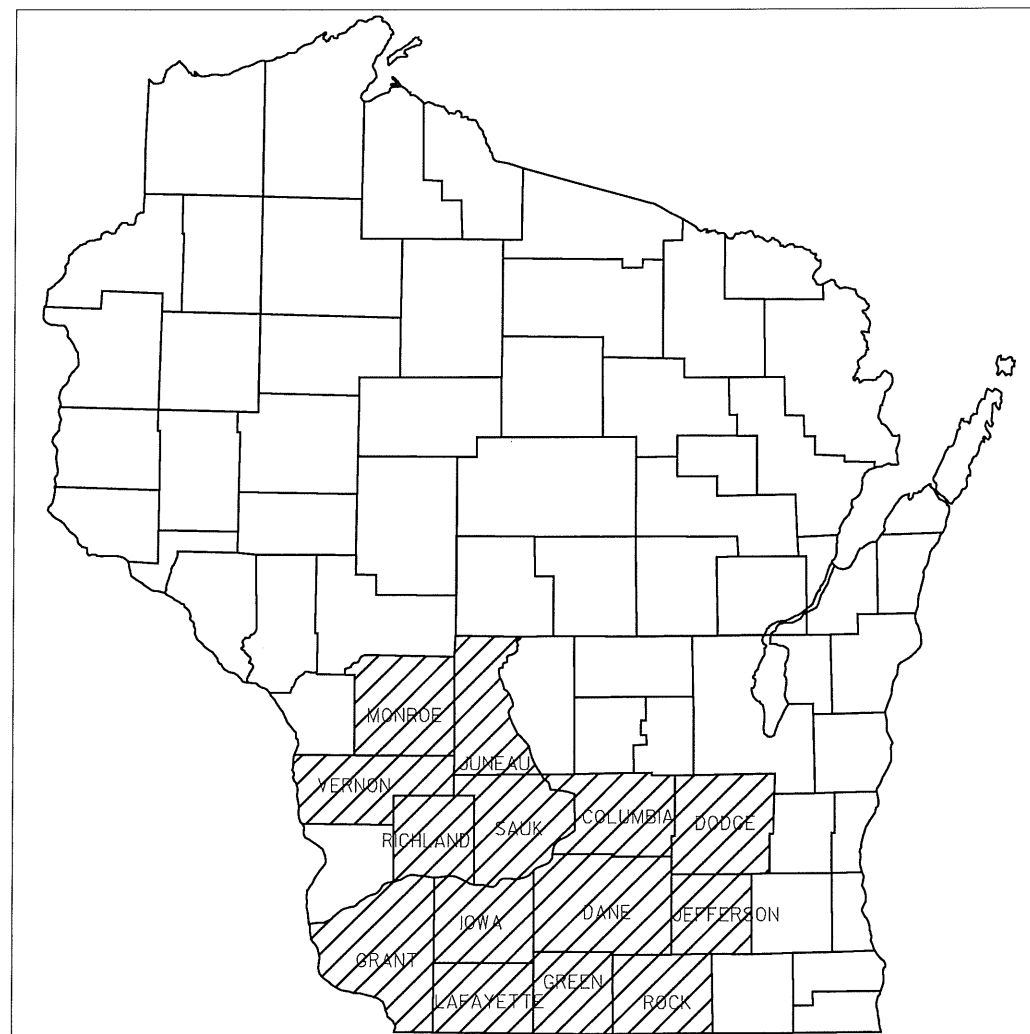
PLAN OF PROPOSED IMPROVEMENT

HORIZONTAL ALIGNMENT & ONE WAY SIGNING

LOCATIONS ON STN PER ANNUAL PLAN

NON HWY
STATEWIDE

STATE PROJECT NUMBER
1000-20-80



LAYOUT
SCALE 0

TOTAL NET LENGTH OF CENTERLINE =

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, COUNTY COUNTY, NAD83 (YEAR), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

STATE PROJECT

FEDERAL PROJECT

PROJECT

CONTRACT

1000-20-80

WISC 2016218

1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor _____
Designer _____ ERIC GLINDINING
Project Manager _____ MATT RAUCH
Regional Examiner _____ JAY HILLE
Regional Supervisor _____ TRAVIS FELTES

APPROVED FOR THE DEPARTMENT


DATE: 4-5-16 Travis Feltes (Signature)

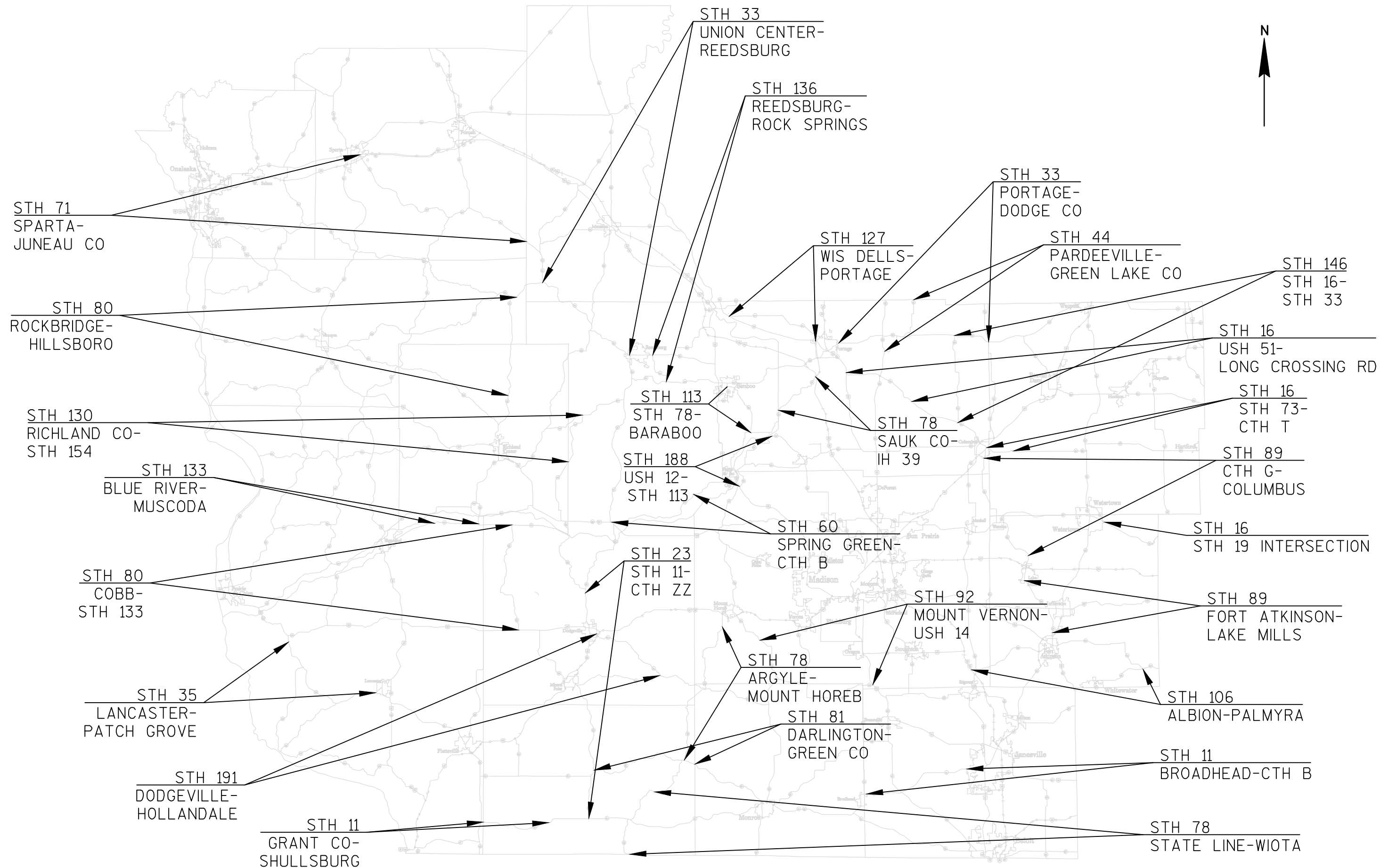
E

GENERAL NOTES

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OR DIGGERS HOTLINE.



Dial  or (800) 242-8511
www.DiggersHotline.com



DATE 06APR16		E S T I M A T E O F Q U A N T I T I E S			
LINE					1000-20-80
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	619.1000	Mobilization	EACH	1.000	1.000
0020	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	262.000	262.000
0030	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	165.000	165.000
0040	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	93.000	93.000
0050	637.2230	Signs Type II Reflective F	SF	4,296.250	4,296.250
0060	638.2102	Moving Signs Type II	EACH	15.000	15.000
0070	638.2602	Removing Signs Type II	EACH	636.000	636.000
0080	638.3000	Removing Small Sign Supports	EACH	540.000	540.000
0090	643.0100	Traffic Control (project) 01.	EACH	1.000	1.000
		1000-20-80			

3

3

Rock County														638.3000						
STH 11 From STH 104 (Brodhead) to CTH WC (Beloit)														634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing
STH 11 EB mi at STH 104 is 72.63. STH 11 WB mi at CTH WC is 9.27														Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small
														4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign
Sign								Order				Reflective F	Type II	Type II	Supports					
Number	Route	Marker	Position	Code	Description	Width	Height	Lines	14 Foot	16 Foot	18 Foot	SF	Each	Each	Each	Notes				
11-1-1R	_011E	75.29 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	New Sign and Post 300' East of Existing 0.28 Miles East of Willing Rd				
11-1-2R	_011E	87.55 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1					
11-1-3	_011E	87.55 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25								
11-1-4R	_011E	88.03 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1					
11-1-5R	_011W	9.33 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1	New Sign and Post 0.24 Miles East of CTH B (West Junction)				
11-1-6R	_011W	9.77 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1					
11-1-7	_011W	15.24 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1			6.25								
11-1-8R	_011W	22.15 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1					
Segment Subtotals									2	0	0	12.50	0	6	6					

Dane and Jefferson Counties										638.3000							
STH 106 From STH 73 (East of Albion) to STH 59 (Palmyra)										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	
STH 106 EB mi at STH 73 is 0.00. STH 106 WB mi at STH 59 is 0.00										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
								Lines		Each	Each	Each	SF	Each	Each	Each	
106-1R	_106E	0.62 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in										
106-2R	_106E	0.62 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45							1	1	Attached to the Same Post as 106-1R
106-3	_106E	0.64 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 100' East of Existing
106-4	_106E	0.64 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				100 Feet West of Hillside Rd
106-5R	_106E	0.87 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-6R	_106E	0.87 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 106-5R
106-7R	_106E	1.22 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
106-8R	_106E	1.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									Attached to the Same Post as 106-7R
106-9	_106E	1.22 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 400' East of Existing
106-10	_106E	1.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				550 Feet West of Liguori Rd
106-11R	_106E	1.78 mi	Right	W1-10-L	Combination Left Curve / Intersection Sign	30.00 in	30.00 in								1	1	
106-12R	_106E	1.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 106-11R
106-13	_106E	1.82 mi	Right	W1-10-L	Combination Left Curve / Intersection Sign	30.00 in	30.00 in				1		6.25				New Sign and Post 150' East of Existing
106-14	_106E	1.82 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				450 Feet West of Bingham Rd (East Junction)
106-15R	_106E	2.08 mi	Left	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
106-16R	_106E	2.08 mi	Left	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									Attached to the Same Post as 106-15R
106-17	_106E	2.09 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in										
106-18R	_106E	2.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25							1		Remove Speed Advisory From Existing Post
106-19	_106E	2.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				New Speed Advisory
106-20R	_106E	2.65 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
106-21R	_106E	2.65 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 106-20R

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

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Dane and Jefferson Counties										638.3000							
STH 106 From STH 73 (East of Albion) to STH 59 (Palmyra) Continued										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	
STH 106 EB mi at STH 73 is 0.00. STH 106 WB mi at STH 59 is 0.00										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
								Lines		Each	Each	Each	SF	Each	Each	Each	
106-22	_106E	2.73 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 350' East of Existing 0.55 Miles East of Bingham Rd (West Junction)
106-23	_106E	2.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
106-24	_106E	2.81 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		
106-25R	_106E	3.05 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
106-26R	_106E	3.29 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 106-26R New Sign and Post 100' East of Existing 0.23 Miles West of Rockdale Rd
106-27R	_106E	3.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35			1		6.25				
106-28	_106E	3.32 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			2.25				
106-29	_106E	3.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					8.00				New Sign and Post 0.15 Miles West of Rockdale Rd
106-30	_106E	3.42 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1								
106-31R	_106E	4.15 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 106-31R New Sign and Post 300' East of Existing 500 Feet West of Loga Rd
106-32R	_106E	4.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35			1		6.25				
106-33	_106E	4.22 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		2.25				
106-34	_106E	4.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					3.00				New Sign and Post 150 Feet West of Loga Rd
106-35	_106E	4.30 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1								
106-36R	_106E	4.31 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	New Sign and Post 50 Feet West of Loga Rd New Sign and Post At Loga Rd New Sign and Post 50 Feet East of Loga Rd
106-37	_106E	4.32 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				
106-38	_106E	4.33 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				
106-39	_106E	4.35 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				
106-40	_106E	4.37 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 150 Feet East of Loga Rd
106-41R	_106E	4.70 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
106-42R	_106E	4.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
106-43	_106E	4.70 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in					1	6.25				
106-44	_106E	4.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
106-45R	_106E	4.95 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	New Sign and Post 600 Feet of Existing
106-46	_106E	5.08 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				
106-47R	_106E	5.55 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
106-48R	_106E	5.89 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-49R	_106E	5.89 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as 106-47R
106-50	_106E	6.00 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 450' East of Existing 0.14 Miles West of Klement Rd
106-51	_106E	6.00 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
106-52R	_106E	6.43 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 106-51R
106-53R	_106E	6.43 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									

3

Dane and Jefferson Counties STH 106 From STH 73 (East of Albion) to STH 59 (Palmyra) Continued STH 106 EB mi at STH 73 is 0.00. STH 106 WB mi at STH 59 is 0.00										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
Sign	Route	Marker	Position	Code	Description	Width	Height	Order	Lines	Posts Wood 4 x 6 - Inch 14 Foot Each	Posts Wood 4 x 6 - Inch 16 Foot Each	Posts Wood 4 x 6 - Inch 18 Foot Each	Signs Type II Reflective F SF	Moving Signs Type II Each	Removing Signs Type II Each	Removing Small Sign Supports Each	
106-54	_106E	6.52 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				
106-55	_106E	6.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				
106-56R	_106E	8.78 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	Attached to the Same Post as 106-55R
106-57R	_106E	8.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									
106-58	_106E	8.94 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 700' East of Existing 400 Feet West of CTH J
106-59	_106E	8.94 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				
106-60	_106E	9.00 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
106-61R	_106E	9.15 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1		Remove Sign From Behind No Passing Zone Sign
106-62R	_106E	10.90 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-63	_106E	11.24 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
106-64R	_106E	18.58 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	Remove Sign From Behind No Passing Zone Sign
106-65R	_106E	24.30 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
106-66R	_106E	26.13 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1		
106-67R	_106E	27.10 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-68R	_106E	27.88 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in								1	1	Attached to the Same Post as 106-67R
106-69R	_106E	27.88 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									
106-70	_106E	27.96 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 400' East of Existing 350 Feet West of First St (Palmyra)
106-71	_106E	27.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25					4.00				
106-72	_106E	28.00 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
106-73	_106E	28.08 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
106-74R	_106W	0.19 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in								1	1	Attached to the Same Post as 106-73R New Sign and Post at Existing Location 50 Feet East of Northwest St (Palmyra)
106-75R	_106W	0.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									
106-76	_106W	0.19 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in					1	9.00				
106-77	_106W	0.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25					4.00				
106-78	_106W	0.27 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			8.00				Remove From Stop Sign and Move to New Post At First St
106-79	_106W	0.33 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			8.00		1		
106-80R	_106W	0.76 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
106-81R	_106W	1.66 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-82R	_106W	3.60 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-83R	_106W	9.38 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
106-84R	_106W	16.74 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	New Sign and Post at Existing Location 600 Feet East of Sinnissippi Dr (Ft Atkinson)
106-85	_106W	16.83 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			8.00		1	1	

Dane and Jefferson Counties STH 106 From STH 73 (East of Albion) to STH 59 (Palmyra) Continued STH 106 EB mi at STH 73 is 0.00. STH 106 WB mi at STH 59 is 0.00									634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
									Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
									4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
									14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
									Each	Each	Each	SF	Each	Each	Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines								
106-86R	_106W	17.17 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
106-87R	_106W	18.84 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
106-88R	_106W	19.11 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00		1	1	
106-89R	_106W	19.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00				Attached to the Same Post as 106-87R
106-90	_106W	19.26 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					8.00		1		New Sign and Post 600' West of Existing
106-91	_106W	19.26 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30								450 Feet East of CTH J
106-92	_106W	19.33 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1		Replace Sign on Existing Post
106-93R	_106W	21.54 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
106-94R	_106W	21.54 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								Attached to the Same Post as 106-92R
106-95	_106W	21.63 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 350' West of Existing
106-96	_106W	21.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25				50 Feet West of Kreutz Rd
106-97R	_106W	22.15 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
106-98R	_106W	22.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								Attached to the Same Post as 106-96R
106-99	_106W	22.22 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25				New Sign and Post 200' West of Existing
106-100	_106W	22.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				100 Feet East of Klement Rd
106-101R	_106W	22.47 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
106-102R	_106W	23.01 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
106-103	_106W	23.01 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 700' West of Existing
106-104R	_106W	23.33 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	300 Feet West of CTH A
106-105R	_106W	23.33 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								Attached to the Same Post as 106-103R
106-106	_106W	23.48 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 600' West of Existing
106-107	_106W	23.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				0.21 Miles West of Hammarquist Rd
106-108	_106W	23.64 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1			6.25				New Sign and Post
106-109R	_106W	23.81 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	0.37 Miles West of Hammarquist Rd
106-110R	_106W	23.81 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								Attached to the Same Post as 106-108R
106-111	_106W	23.92 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 350' West of Existing
106-112	_106W	23.92 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25				600 Feet East of Loga Rd
106-113	_106W	23.99 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post
106-114	_106E	24.01 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1							150 Feet East of Loga Rd
106-115R	_106W	24.02 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1	1	New Sign and Post
106-116	_106W	24.02 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				50 Feet East of Loga Rd
106-117	_106W	24.04 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1							New Sign and Post
106-118	_106W	24.06 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				50 Feet West of Loga Rd
																New Sign and Post
																150 Feet West of Loga Rd

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

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Dane and Jefferson Counties										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 106 From STH 73 (East of Albion) to STH 59 (Palmyra) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 106 EB mi at STH 73 is 0.00. STH 106 WB mi at STH 59 is 0.00										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign								Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Signs	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	Notes
106-119R	_106W	24.16 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
106-120R	_106W	24.16 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 106-118R
106-121	_106W	24.22 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 250' West of Existing
106-122	_106W	24.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				0.18 Miles East of Conrad Rd
106-123	_106W	24.30 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
106-124R	_106W	24.68 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
106-125R	_106W	24.68 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									Attached to the Same Post as 106-125R
106-126	_106W	24.92 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 50' East of Existing
106-127R	_106W	24.93 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	550 Feet West of Rockdale Rd
106-128R	_106W	24.98 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-129R	_106W	25.35 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-130R	_106W	25.35 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 106-128R
106-131	_106W	25.45 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 400' West of Existing
106-132	_106W	25.45 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.65 Miles West of Rockdale Rd
106-133R	_106W	26.03 mi	Left	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	
106-134R	_106W	26.03 mi	Left	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									Attached to the Same Post as 106-132R
106-135	_106W	26.27 mi	Right	W1-10-R	Combination Right Curve / Intersection Sign	30.00 in	30.00 in										
106-136R	_106W	26.27 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40							1		Remove Speed Advisory From Existing Post
106-137	_106W	26.27 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Speed Advisory
106-138R	_106W	26.59 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
106-139R	_106W	26.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									Attached to the Same Post as 106-137R
106-140	_106W	26.69 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 300' West of Existing
106-141	_106W	26.69 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				600 Feet East of Silver Ln
106-142	_106W	26.95 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
106-143R	_106W	27.20 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	At Liguori Rd Southw est Corner
106-144R	_106W	27.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 106-142R
106-145R	_106W	27.47 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
106-146R	_106W	27.47 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as 106-144R
106-147	_106W	27.52 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 200' West of Existing
106-148	_106W	27.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				0.18 Miles East of Hillside Rd
Segment Subtotals										19	16	5	351.00	0	59	47	

Jefferson County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 16 at STH 19										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 16 EB mi at STH 19 is 110.82.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign								Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Signs	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	Notes
16-2-1R	_016E	111.00 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in									1	1
16-2-2R	_016E	111.00 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									Attached to the Same Post as Sign 16-2-1R
Segment Subtotals										0	0	0	0	0	1	1	

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

FILE NAME : _____

PLOT DATE : _____

PLOT BY : _____

PLOT NAME : _____

ORG DATE : _____

ORIGINATOR : DIST _

PLOT SCALE : 1:1

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Jefferson County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 89 From USH 12 (Ft Atkinson) to CTH A South Junction (Lake Mills)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 89 NB mi at USH 12 is 0.00. STH 89 SB mi at CTH A (South Junction) is 22.66										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign								Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Signs	Notes
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	
89-1R	_089N	1.53 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
89-2R	_089N	1.53 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 89-1R
89-3	_089N	1.66 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 550' North of Existing 0.45 Miles North of Hoard Rd
89-4R	_089N	3.26 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
89-5M	_089N	3.31 mi	Right	J4-1	Reassurance Assembly	24.00 in	36.00 in	NORTH 89						1			Move Sign and Post 200' South 100 Feet North of CTH W
89-6	_089N	3.32 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 250' North of Existing 400 Feet North of CTH W
89-7R	_089N	4.56 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
89-8R	_089N	4.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 89-7R
89-9	_089N	4.62 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 150' North of Existing 0.33 Miles North of CTH J
89-10	_089N	4.62 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
89-11R	_089N	10.04 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
89-12R	_089S	28.21 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
89-13R	_089S	28.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 89-12R
89-14	_089S	28.21 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 300' South of Existing 0.33 Miles North of CTH J
89-15	_089S	28.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
89-16R	_089S	29.38 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
89-17R	_089S	29.38 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 89-16R
89-18	_089S	29.47 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 350' South of Existing 0.40 Miles North of CTH W
89-19R	_089S	31.14 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
89-20R	_089S	31.14 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 89-19R
89-21	_089S	31.20 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 250' South of Existing 0.68 Miles North of Hoard Rd
Segment Subtotals										4	2	0	42.00	1	7	7	

Jefferson, Dane, Dodge and Columbia Counties										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 89 From CTH G (North of Lake Mills) to STH 73 (Columbus)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 89 NB mi at CTH G is 14.23. STH 89 SB mi at STH 73 is 0.00										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign								Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Signs	Notes
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	
89-1-1M	_089N	17.95 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1			Move Sign and Post 450' North From Existing Location 0.53 Miles North of Lenius Ln
89-1-2	_089N	19.50 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 0.21 Miles South of Dunnisen Ln
89-1-3	_089N	20.14 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 550 Feet South of Island Rd
89-1-4R	_089N	21.37 mi	Island Left Side	W1-8	Chevron	18.00 in	24.00 in								1	1	
89-1-5R	_089N	21.37 mi	Island Left Side	W1-8	Chevron	18.00 in	24.00 in								1	1	
89-1-6R	_089N	21.37 mi	Island Left Side	W1-8	Chevron	18.00 in	24.00 in								1	1	

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

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Jefferson, Dane, Dodge and Columbia Counties															638.3000	
STH 89 From CTH G (North of Lake Mills) to STH 73 (Columbus) Continued															Removing Small Sign Supports	
STH 89 NB mi at CTH G is 14.23. STH 89 SB mi at STH 73 is 0.00																
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
89-1-7	_089N	21.37 mi	Island Left Side	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00				New Sign and Post 50 Feet South of STH 19
89-1-8R	_089N	24.94 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-8R
89-1-9R	_089N	24.94 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								
89-1-10	_089N	25.01 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 350' North of Existing 700 Feet South of Tri County Rd
89-1-11	_089N	25.01 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25				
89-1-12R	_089N	26.96 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-12R
89-1-13R	_089N	26.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50								
89-1-14R	_089N	27.21 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-14R
89-1-15R	_089N	27.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								
89-1-16	_089N	27.24 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 150' North of Existing 0.25 Miles South of Muller Rd
89-1-17	_089N	27.24 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25				
89-1-18R	_089N	27.36 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1	1	New Sign and Post 25' North of Existing 550 Feet South of Muller Rd
89-1-19	_089N	27.36 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00				
89-1-20	_089N	29.68 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 100' South of Existing 0.18 Miles North of Sydo Rd
89-1-21	_089N	29.68 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25				
89-1-22R	_089N	29.70 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-22R
89-1-23R	_089N	29.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								
89-1-24R	_089N	30.18 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in							1	1	New Sign and Post 100' North of Existing 0.38 Miles South of Columbus Rd
89-1-25R	_089N	30.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								
89-1-26	_089N	30.21 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 500 Feet South of Columbus Rd
89-1-27	_089N	30.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25				
89-1-28	_089N	30.49 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1		6.25				Attached to the Same Post as Sign 89-1-30R
89-1-29	_089N	30.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
89-1-30R	_089N	31.48 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-30R
89-1-31R	_089N	31.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50								
89-1-32R	_089S	1.10 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-32R
89-1-33R	_089S	1.10 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50								
89-1-34	_089S	1.20 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 450' South of Existing 0.20 Miles North of Fox Rd
89-1-35	_089S	1.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
89-1-36R	_089S	2.32 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-36R
89-1-37R	_089S	2.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								
89-1-38	_089S	2.36 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 200' South of Existing 550 Feet North of Columbus Rd
89-1-39	_089S	2.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
89-1-40	_089S	2.60 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 650 Feet South of Columbus Rd
89-1-41	_089S	2.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
89-1-42R	_089S	3.05 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 89-1-42R
89-1-43R	_089S	3.05 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								

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Jefferson, Dane, Dodge and Columbia Counties										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
STH 89 From CTH G (North of Lake Mills) to STH 73 (Columbus) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 89 NB mi at CTH G is 14.23. STH 89 SB mi at STH 73 is 0.00										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number								Lines		Each	Each	Each	SF	Each	Each	Each	
89-1-44	_089S	3.06 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 50' South of Existing
89-1-45	_089S	3.06 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				0.40 Miles North of Sydo Rd
89-1-46R	_089S	5.46 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as Sign 89-1-46R
89-1-47R	_089S	5.46 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
89-1-48	_089S	5.56 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 400' South of Existing
89-1-49	_089S	5.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				100 Feet North of Muller Rd
89-1-50	_089S	5.65 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
89-1-51R	_089S	5.69 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as Sign 89-1-51R
89-1-52R	_089S	5.69 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									
89-1-53R	_089S	7.72 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as Sign 89-1-53R
89-1-54R	_089S	7.72 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									
89-1-55	_089S	7.80 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 400' South of Existing
89-1-56	_089S	7.80 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				100 Feet South of Sun Prairie Rd
89-1-57R	_089S	8.60 mi	Left	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20							1		Replace Existing Speed Advisory
89-1-58	_089S	8.60 mi	Left	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15					4.00				
89-1-59R	_089S	8.61 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00		1		Replace Existing Speed Advisory
89-1-60	_089S	8.61 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15									
89-1-61M	_089S	14.76 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1			Move Sign and Post 450' South From Existing Location
Segment Subtotals										3	12	0	138.00	2	19	16	0.61 Miles South of Tow n Hall Rd

Dodge County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
STH 16 From STH 73 (Columbus) to CTH T										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 16 EB mi at STH 73 is 83.96. STH 16 WB mi at CTH T is 31.81										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number								Lines		Each	Each	Each	SF	Each	Each	Each	
16-1R	_016E	84.95 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
16-2R	_016E	87.18 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
16-3R	_016W	31.84 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
16-4R	_016W	34.43 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
Segment Subtotals										0	0	0	0.00	0	4	4	

Columbia County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
STH 16 From USH 51 (South of Portage) to Long Crossing Rd (East of Rio)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 16 EB mi at USH 51 is 59.88. STH 16 WB mi at Long Crossing Rd is 48.84										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number								Lines		Each	Each	Each	SF	Each	Each	Each	
16-1-1R	_016W	49.25 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
16-1-2	_016W	59.42 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			8.00				New Sign and Post
Segment Subtotals										1	0	0	8.00	0	1	1	150 Feet East of Murray Rd

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

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Columbia County										638.3000													
STH 146 From STH 16 (East of Fall River) to STH 33 (North of Cambria)										634.0614		634.0616		634.0618		637.2230		638.2102		638.2602		Removing	
STH 146 NB mi at STH 16 is 0.00. STH 146 SB mi at STH 33 is 0.00.										Posts Wood		Posts Wood		Posts Wood		Signs		Moving		Removing		Small	
										4 x 6 - Inch		4 x 6 - Inch		4 x 6 - Inch		Type II		Signs		Signs		Sign	
Sign										14 Foot		16 Foot		18 Foot		Reflective F		Type II		Type II		Supports	
Number	Route	Marker	Position	Code	Description	Width	Height	Order	Lines	Each	Each	Each	SF	Each	Each	Each	Notes						
146-1R	_146N	3.43 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in																
146-2R	_146N	3.43 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								1	1	Attached to the Same Post as Sign 146-1R					
146-3	_146N	3.53 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 450' North of Existing						
146-4	_146N	3.53 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				0.22 Miles North of Rupnow Rd						
146-5	_146N	3.61 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			8.00				New Sign and Post						
146-6	_146N	3.76 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				0.30 Miles North of Rupnow Rd						
146-7R	_146N	3.92 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in									1	1	New Sign and Post					
146-8R	_146N	3.92 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	30									0.45 Miles North of Rupnow Rd						
146-9R	_146N	3.92 mi	Right	W8-8	Rough Road	30.00 in	30.00 in										Attached to the Same Post as Sign 146-7R						
																	Attached to the Same Post as Sign 146-7R						
146-10	_146N	4.01 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in					1	6.25				New Sign and Post 500' North of Existing						
146-11	_146N	4.01 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				0.22 Miles North of Rupnow Rd						
146-12	_146N	4.07 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00			1	Replace Sign on Existing Post						
146-13	_146N	4.48 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post						
146-14	_146N	4.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				100 Feet South of Shady Ln						
146-15	_146N	4.54 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post						
146-16	_146N	4.56 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				200 Feet North of Shady Ln						
146-17R	_146N	4.58 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in									1	1	New Sign and Post					
146-18	_146N	4.58 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				300 Feet North of Shady Ln						
																	New Sign and Post at Existing Location						
146-19	_146N	4.60 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				400 Feet North of Shady Ln						
146-20	_146N	4.63 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				500 Feet North of Shady Ln						
146-21	_146N	4.65 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post						
																	750 Feet South of Palmer Rd						
																	New Sign and Post						
																	650 Feet South of Palmer Rd						
146-22R	_146N	4.67 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			3.00			1	1	New Sign and Post					
146-23	_146N	4.67 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				550 Feet South of Palmer Rd						
146-24	_146N	4.69 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post						
146-25	_146N	4.71 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				450 Feet South of Palmer Rd						
																	New Sign and Post						
																	350 Feet South of Palmer Rd						
146-26R	_146N	5.12 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in									1	1	New Sign and Post at Existing Location					
146-27R	_146N	5.12 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									550 Feet South of Palmer Rd						
146-28	_146N	5.18 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 350' North of Existing						
146-29	_146N	5.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.41 Miles North of Palmer Rd						
146-30	_146N	5.26 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00			1	Replace Sign on Existing Post						
146-31R	_146N	5.63 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in									1	1						
146-32R	_146N	7.06 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in									1	1						
146-33R	_146N	7.06 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 146-32R						
146-34	_146N	7.06 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 500' North of Existing						
146-35	_146N	7.06 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				750 Feet South of CTH A (South Junction)						

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Columbia County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 146 From STH 16 (East of Fall River) to STH 33 (North of Cambria) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 146 NB mi at STH 16 is 0.00. STH 146 SB mi at STH 33 is 0.00.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign									Order	14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	Notes
146-36R	_146N	7.51 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
146-37R	_146N	7.51 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 146-36R
146-38	_146N	7.61 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 450' North of Existing
146-39	_146N	7.61 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				900 Feet South of CTH A (North Junction)
146-40	_146N	7.69 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
146-41	_146N	10.58 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in						6.25		1		Replace Signs on Existing Post
146-42	_146N	10.58 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				
146-43	_146N	10.65 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
146-44	_146N	11.51 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in						9.00		1		Replace Signs on Existing Post
146-45	_146N	11.51 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00				
146-46	_146N	11.83 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00		1	1	Replace Sign and Post at Existing Location
146-47	_146N	11.83 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00				New Sign Under Sign 146-46
146-48	_146N	11.90 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
146-49R	_146N	12.03 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
146-50R	_146N	12.03 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									Attached to the Same Post as Sign 146-49R
146-51	_146N	12.08 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 200' North of Existing
146-52	_146N	12.08 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				100 Feet South of Highland Dr (Cambria)
146-53	_146N	12.19 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-54	_146N	12.21 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				450 Feet North of Highland Dr (Cambria)
146-55	_146N	12.23 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-56	_146N	12.25 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				650 Feet North of Highland Dr (Cambria)
146-57	_146N	12.27 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-58R	_146S	0.80 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	750 Feet North of Highland Dr (Cambria)
146-59R	_146S	0.80 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									New Sign and Post
146-60	_146S	0.88 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				850 Feet North of Highland Dr (Cambria)
146-61	_146S	0.88 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign and Post
146-62	_146S	0.95 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				750 Feet North of Highland Dr (Cambria)
146-63	_146S	0.97 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-64	_146S	0.99 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				450 Feet North of Highland Dr (Cambria)
146-65	_146S	1.01 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-66	_146S	1.03 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				550 Feet North of Highland Dr (Cambria)
146-67R	_146S	1.09 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	New Sign and Post
146-68R	_146S	1.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									450 Feet North of Highland Dr (Cambria)
										Attached to the Same Post as Sign 146-67R							

Columbia County STH 146 From STH 16 (East of Fall River) to STH 33 (North of Cambria) Continued STH 146 NB mi at STH 16 is 0.00. STH 146 SB mi at STH 33 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									Notes
146-69	_146S	1.09 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00					
146-70	_146S	1.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00					
146-71	_146S	1.28 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1			Replace Sign on Existing Post
146-72	_146S	1.50 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in					9.00		1			Replace Signs on Existing Post
146-73	_146S	1.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20				4.00					
146-74	_146S	1.55 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1			Replace Sign on Existing Post
146-75R	_146S	1.55 mi	Left	W1-8	Chevron	18.00 in	24.00 in							1	1		
146-76R	_146S	1.55 mi	Left	W1-8	Chevron	18.00 in	24.00 in							1	1		
146-77R	_146S	1.55 mi	Left	W1-8	Chevron	18.00 in	24.00 in							1	1		
146-78R	_146S	1.55 mi	Left	W1-8	Chevron	18.00 in	24.00 in							1	1		
146-79R	_146S	1.56 mi	Left	W1-8	Chevron	18.00 in	24.00 in							1	1		
146-80R	_146S	1.56 mi	Left	W1-8	Chevron	18.00 in	24.00 in							1	1		
146-81R	_146S	2.12 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in							1	1		
146-82R	_146S	2.43 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1		
146-83R	_146S	2.43 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35			1	6.25					Attached to the Same Post as Sign 146-82R
146-84	_146S	2.43 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in					2.25					New Sign and Post at Existing Location
146-85	_146S	2.43 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									50 Feet North of Cabbage Rd (Cambria)
146-86R	_146S	5.15 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1		
146-87R	_146S	5.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40			1	6.25					Attached to the Same Post as Sign 146-86R
146-88	_146S	5.15 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in					2.25					New Sign and Post at Existing Location
146-89	_146S	5.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									0.18 Miles South of Hughes Rd
146-90	_146S	5.35 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1			Replace Sign on Existing Post
146-91R	_146S	5.65 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1		
146-92R	_146S	5.65 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40			1	6.25					Attached to the Same Post as Sign 146-91R
146-93	_146S	5.72 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1	2.25					New Sign and Post 300' South of Existing
146-94	_146S	5.72 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									0.20 Miles North of CTH A (South Junction)
146-95	_146S	5.83 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					New Sign and Post
146-96R	_146S	7.18 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1		450 Feet North of CTH A (South Junction)
146-97R	_146S	7.73 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1		
146-98R	_146S	7.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40			1	6.25					Attached to the Same Post as Sign 146-97R
146-99	_146S	7.86 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	2.25					New Sign and Post 450' South of Existing
146-100	_146S	7.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									0.40 Miles North of Palmer Rd
146-101	_146S	7.92 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1			Replace Sign on Existing Post
146-102R	_146S	8.37 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in							1	1		
146-103R	_146S	8.37 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	30									Attached to the Same Post as Sign 146-102R

3

Columbia County										638.3000							
STH 146 From STH 16 (East of Fall River) to STH 33 (North of Cambria) Continued										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	
STH 146 NB mi at STH 16 is 0.00. STH 146 SB mi at STH 33 is 0.00.										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign									Order	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
										Each	Each	Each	SF	Each	Each	Each	
146-104	_146S	8.42 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 150' South of Existing
146-105	_146S	8.42 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				200 Feet North of Palmer Rd
146-106	_146S	8.51 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-107R	_146S	8.55 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	350 Feet South of Palmer Rd
146-108	_146S	8.53 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post at Existing Location
146-109	_146S	8.55 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				450 Feet South of Palmer Rd
146-110	_146S	8.57 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-111	_146S	8.59 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				550 Feet South of Palmer Rd
146-112	_146S	8.63 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-113	_146S	8.65 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				500 Feet North of Shady Ln
146-114R	_146S	8.67 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	New Sign and Post
146-115	_146S	8.67 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				750 Feet South of Palmer Rd
146-116	_146S	8.69 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
146-117	_146S	8.73 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in				1		6.25				200 Feet North of Shady Ln
146-118	_146S	8.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in						2.25				New Sign and Post
146-119	_146S	8.81 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			8.00				50 Feet North of Shady Ln
146-120R	_146S	8.88 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	New Sign and Post
146-121R	_146S	9.04 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	300 Feet South of Shady Ln
146-122R	_146S	9.04 mi	Right	W8-8	Rough Road	30.00 in	30.00 in										Attached to the Same Post as Sign 146-121R
146-123	_146S	9.35 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post
146-124R	_146S	9.39 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	0.59 Miles North of Rupnow Rd
146-125R	_146S	9.39 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 146-124R
146-126	_146S	9.51 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 500' South of Existing
146-127	_146S	9.51 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.43 Miles North of Rupnow Rd
Segment Subtotals										33	14	3	380.50	0	42	30	

3

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

FILE NAME : _____

PLOT DATE : _____

PLOT BY : _____

PLOT NAME : _____

ORG DATE : _____

ORIGINATOR : DIST _

PLOT SCALE : 1:1

3

Columbia County															
STH 33 From USH 51 and STH 16 (Portage) to STH 73 (North of Randolph)															
STH 33 EB mi at USH 51 and STH 16 is 102.24. STH 33 WB mi at STH 73 is 37.22.															
Sign									634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000
Number	Route	Marker	Position	Code	Description	Width	Height	Order	Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing
								Lines	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign
									14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports
									Each	Each	Each	SF	Each	Each	Each
															Notes
33-1-1R	_033E	102.98 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
33-1-2R	_033E	103.35 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1
33-1-3R	_033E	106.71 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
33-1-4R	_033E	108.15 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
33-1-5R	_033E	109.18 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1
33-1-6R	_033E	112.29 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
33-1-7R	_033E	113.22 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
33-1-8R	_033E	113.60 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1
33-1-9R	_033E	114.75 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
33-1-10	_033W	41.98 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1	Replace Sign on Existing Post
33-1-11R	_033W	48.19 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
33-1-12R	_033W	49.11 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1
33-1-13R	_033W	49.81 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
33-1-14R	_033W	50.72 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
33-1-15R	_033W	53.53 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1
33-1-16R	_033W	54.74 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
33-1-17	_033W	54.88 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25			New Sign and Post 600' West of Existing
33-1-18R	_033W	56.21 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
33-1-19R	_033W	59.21 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1
33-1-20R	_033W	59.93 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
33-1-21R	_033W	60.39 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
Segment Subtotals									1	0	0	14.25	0	20	19

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PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

FILE NAME : _____

PLOT DATE : _____

PLOT BY : _____

PLOT NAME : _____

ORG DATE : _____

ORIGINATOR : DIST _

PLOT SCALE : 1:1

3

Columbia County
STH 44 From STH 22 (Pardeeville) to Green Lake County
STH 44 NB mi at STH 22 is 0.00. STH 44 SB mi at Green Lake County is 0.00.

Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
44-1R	_044N	0.68 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1	1	
44-2R	_044N	3.36 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1	
44-3R	_044N	3.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50								Attached to the Same Post as Sign 44-3R
44-4	_044N	3.48 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 550' North of Existing
44-5	_044N	3.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				0.28 Miles North of STH 33
44-6R	_044N	4.81 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1	
44-7R	_044N	4.81 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								Attached to the Same Post as Sign 44-6R
44-8	_044N	4.88 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00				New Sign and Post 400' North of Existing
44-9	_044N	4.88 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00				0.28 Miles North of STH 33
44-10	_044N	4.97 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 150 Feet South of Barden Rd
44-11	_044N	4.99 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 50 Feet South of Barden Rd
44-12R	_044N	5.01 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1	1	
44-13	_044N	5.01 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post at Existing Location 50 Feet North of Barden Rd
44-14	_044N	5.03 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 150 Feet North of Barden Rd
44-15	_044N	5.05 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 250 Feet North of Barden Rd
44-16R	_044N	5.05 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1	
44-17R	_044N	5.05 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45								Attached to the Same Post as Sign 44-16R
44-18	_044N	5.11 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25				New Sign and Post 250' North of Existing
44-19	_044N	5.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25				550 Feet North of Barden Rd
44-20	_044N	5.11 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00				New Sign and Post 750 Feet North of Barden Rd
44-21R	_044N	5.17 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1	
44-22R	_044N	5.17 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35								Attached to the Same Post as Sign 44-21R
44-23	_044N	5.19 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 100' North of Existing
44-24	_044N	5.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25				0.19 Miles North of Barden Rd
44-25	_044N	5.26 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 0.26 Miles North of Barden Rd
44-26	_044N	5.28 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 0.28 Miles North of Barden Rd
44-27	_044N	5.30 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 0.30 Miles North of Barden Rd
44-28	_044N	5.32 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 0.32 Miles North of Barden Rd
44-29	_044N	5.34 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 0.34 Miles North of Barden Rd
44-30R	_044N	6.91 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in							1	1	
44-31R	_044N	6.91 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30								Attached to the Same Post as Sign 44-30R
44-32	_044N	6.95 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00				New Sign and Post 200' North of Existing
44-33	_044N	6.95 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00				700 Feet South of CTH E (South Junction)
44-34R	_044N	7.14 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1	
44-35R	_044N	7.14 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30								Attached to the Same Post as Sign 44-34R

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

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3

Columbia County STH 44 From STH 22 (Pardeeville) to CTH HH (Green Lake County) Continued STH 44 NB mi at STH 22 is 0.00. STH 44 SB mi at CTH HH is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
44-36	_044N	7.16 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 100' North of Existing
44-37	_044N	7.16 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25					4.00				350 Feet North of CTH E (South Junction)
44-38R	_044S	2.05 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	
44-39R	_044S	2.05 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									Attached to the Same Post as Sign 44-38R
44-40	_044S	2.09 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 100' North of Existing
44-41	_044S	2.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25					4.00				350 Feet South of CTH E (North Junction)
44-42R	_044S	3.75 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
44-43R	_044S	3.75 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as Sign 44-42R
44-44	_044S	3.90 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 650' South of Existing
44-45	_044S	3.90 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				0.60 Miles North of Barden Rd
44-46	_044S	3.98 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post
44-47	_044S	4.00 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				0.34 Miles North of Barden Rd
44-48R	_044S	4.02 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	New Sign and Post
44-49	_044S	4.02 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				0.30 Miles North of Barden Rd
44-50	_044S	4.04 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post
44-51	_044S	4.06 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				0.28 Miles North of Barden Rd
44-52R	_044S	4.06 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	New Sign and Post
44-53R	_044S	4.06 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									0.26 Miles North of Barden Rd
44-54	_044S	4.10 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				Attached to the Same Post as Sign 44-52R
44-55	_044S	4.10 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign and Post 150' South of Existing
44-56	_044S	4.15 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				0.20 Miles North of Barden Rd
44-57R	_044S	4.19 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	New Sign and Post
44-58R	_044S	4.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									800 Feet North of Barden Rd
44-59	_044S	4.19 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				Attached to the Same Post as Sign 44-57R
44-60	_044S	4.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				New Sign and Post at Existing Location
44-61	_044S	4.24 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				550 Feet North of Barden Rd
44-62R	_044S	4.26 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	New Sign and Post
44-63	_044S	4.26 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				250 Feet North of Barden Rd
44-64	_044S	4.28 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post at Existing Location
44-65	_044S	4.30 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				150 Feet North of Barden Rd
44-66	_044S	4.32 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post
44-67R	_044S	5.58 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	50 Feet North of Barden Rd
44-68R	_044S	5.58 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									New Sign and Post
44-69	_044S	5.66 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				50 Feet South of Barden Rd
44-70	_044S	5.66 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign and Post
44-71R	_044S	8.57 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	150 Feet South of Barden Rd
Segment Subtotals										22	5	6	192.00	0	16	16	

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Columbia County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 127 From STH 16 (East of Wisconsin Dells) to IH 39 (Portage)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 127 EB mi at STH 16 is 0.00. STH 127 WB mi at IH 39 is 0.00.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign									Order	14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	
127-1R	_127E	2.70 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
127-2R	_127E	2.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 127-1R
127-3	_127E	2.81 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 500' East of Existing
127-4	_127E	2.81 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.36 Miles East of New Haven Rd
127-5R	_127E	3.02 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
127-6R	_127E	3.02 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									On the Same Post as 127-5R
127-7	_127E	3.11 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in					1	6.25				New Sign and Post 350' East of Existing
127-8	_127E	3.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				500 Feet West of Zeitz Rd
127-9R	_127E	7.22 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
127-10R	_127E	7.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 127-9R
127-11	_127E	7.36 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 600' East of Existing
127-12	_127E	7.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				650 Feet West of CTH XX
127-13R	_127E	7.57 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in					1	6.25		1	1	On the Same Post as 127-13R
127-14R	_127E	7.57 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign and Post at Existing Location
127-15	_127E	7.57 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in						6.25				350 Feet West of CTH AA
127-16	_127E	7.57 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
127-17R	_127E	8.94 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in					1	6.25		1	1	On the Same Post as 127-17R
127-18R	_127E	8.94 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45					2.25				New Sign and Post at Existing Location
127-19	_127E	8.94 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in						6.25				0.16 miles East of Weyh Rd
127-20	_127E	8.94 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
127-21	_127E	9.13 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
127-22R	_127E	9.62 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	On the Same Post as 127-22R
127-23R	_127E	9.62 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									
127-24R	_127E	10.69 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
127-25R	_127E	11.21 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
127-26R	_127E	12.86 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	On the Same Post as 127-26R
127-27R	_127E	12.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									
127-28	_127E	12.86 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post at Existing Location
127-29	_127E	12.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.16 miles East of Weyh Rd
127-30	_127E	13.03 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
127-31R	_127E	13.59 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	On the Same Post as 127-31R
127-32R	_127E	13.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									
127-33R	_127W	0.86 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	On the Same Post as 127-33R
127-34R	_127W	0.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									
127-35	_127W	0.97 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 400' West of Existing
127-36	_127W	0.97 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				300 Feet East of Bulgrin Rd
127-37	_127W	1.04 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
127-38R	_127W	2.42 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	

Columbia County STH 127 From STH 16 (East of Wisconsin Dells) to IH 39 (Portage) Continued STH 127 EB mi at STH 16 is 0.00. STH 127 WB mi at IH 39 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									Notes
127-39R	_127W	3.05 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
127-40R	_127W	4.16 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
127-41R	_127W	4.16 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									On the Same Post as 127-40R
127-42R	_127W	4.64 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
127-43R	_127W	4.64 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									On the Same Post as 127-42R
127-44	_127W	4.75 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 400' West of Existing
127-45	_127W	4.75 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					300 Feet East of Simonson Rd
127-46	_127W	4.98 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00			1		Replace Sign on Existing Post
127-47R	_127W	6.25 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
127-48R	_127W	6.25 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 127-47R
127-49	_127W	6.40 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 650' West of Existing
127-50	_127W	6.40 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					300 Feet East of CTH AA
127-51R	_127W	6.50 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
127-52R	_127W	6.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									On the Same Post as 127-51R
127-53	_127W	6.50 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25					New Sign and Post at Existing Location
127-54	_127W	6.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					250 Feet West of CTH AA
127-55	_127W	6.67 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					New Sign and Post
127-56R	_127W	10.60 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	CTH XX (Northw est Corner)
127-57R	_127W	10.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 127-56R
127-58	_127W	10.87 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post
127-59	_127W	10.87 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					350 Feet East of Zeitz Rd
172-60R	_127W	10.96 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
127-61R	_127W	10.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 127-60R
127-62	_127W	10.96 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in				1	6.25					New Sign and Post 200' West of Existing
127-63	_127W	10.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25					400 Feet West of Zeitz Rd
127-64	_127W	14.12 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00			1		Replace Sign on Existing Post
Segment Subtotals										1	7	5	150.00	0	24	19	

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Columbia County STH 78 From CTH DL (North of Merrimac) to IH 39 (South of Portage) STH 78 NB mi at CTH DL is 77.44. STH 78 SB mi at IH 39 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									Notes
78-1R	_078N	77.34 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in								1	1	
78-2R	_078N	77.34 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									On the Same Post as 78-1R
78-3	_078N	77.39 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00					New Sign and Post 200' North of Existing
78-4	_078N	77.39 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00					250 Feet South of CTH DL
78-5R	_078N	77.52 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
78-6R	_078N	77.84 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-7R	_078N	77.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 78-6R
78-8	_078N	77.88 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 150' North of Existing
78-9	_078N	77.88 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					450 Feet South of Blau Dr
78-10	_078N	77.96 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					New Sign and Post 50 Feet South of Blau Dr
78-11R	_078N	78.20 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
78-12R	_078N	78.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 78-11R
78-13	_078N	78.34 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 650' North of Existing
78-14	_078N	78.34 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					0.37 Miles South of Durwards Glen Rd
78-15	_078N	78.43 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00			1		Replace Sign on Existing Post
78-16	_078N	78.65 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25					New Sign and Post
78-17	_078N	78.65 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					300 Feet South of Durwards Glen Rd
78-18R	_078N	80.42 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
78-19R	_078N	80.42 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									On the Same Post as 78-18R
78-20	_078N	80.42 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00					New Sign and Post at Existing Location
78-21	_078N	80.42 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25				4.00					0.19 Miles south of Messner Rd
78-22	_078N	80.53 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00					New Sign and Post 80 Feet South of South Existing Chevron
78-23R	_078N	81.05 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	400 Feet South of Messner Rd
78-24	_078N	81.14 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1			6.25					New Sign and Post 650' North of Existing
78-25R	_078N	81.39 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	100 Feet North of Petra Rd
78-26R	_078N	81.39 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									On the Same Post as 78-25R
78-27	_078N	81.47 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25					New Sign and Post 300' North of Existing
78-28R	_078N	81.56 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	0.35 Miles North of Petra Rd
78-29R	_078N	81.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									On the Same Post as 78-28R
78-30	_078N	81.56 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00					New Sign and Post at Existing Location
78-31	_078N	81.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25				4.00					0.45 Miles North of Petra Rd
78-32R	_078N	81.93 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-33R	_078N	81.93 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									On the Same Post as 78-32R
78-34	_078N	82.11 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 800' North of Existing
78-35	_078N	82.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25					0.23 Miles South of Walker Rd

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PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Columbia County STH 78 From CTH DL (North of Merrimac) to IH 39 (South of Portage) Continued STH 78 NB mi at CTH DL is 77.44. STH 78 SB mi at IH 39 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
78-36R	_078N	82.23 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
78-37R	_078N	82.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									On the Same Post as 78-36R
78-38	_078N	82.23 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00				New Sign and Post at Existing Location
78-39	_078N	82.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				500 Feet South of Walker Rd
78-40R	_078N	82.41 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	On the Same Post as 78-40R
78-41R	_078N	82.41 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									
78-42	_078N	82.49 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 350' North of Existing
78-43	_078N	82.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				0.15 Miles North of Walker Rd
78-44	_078N	82.58 mi	Left	W1-8	Chevron				1				3.00				New Sign and Post
78-45	_078N	82.60 mi	Left	W1-8	Chevron				1				3.00				0.24 Miles North of Walker Rd
78-46	_078N	82.62 mi	Left	W1-8	Chevron				1				3.00				New Sign and Post
																	0.26 Miles North of Walker Rd
																	New Sign and Post
																	0.28 Miles North of Walker Rd
78-47R	_078N	83.15 mi	Left	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	
78-48R	_078N	83.15 mi	Left	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									On the Same Post as 78-47R
78-49R	_078N	83.15 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	
78-50R	_078N	83.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									On the Same Post as 78-49R
78-51	_078N	83.19 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 150' North of Existing
78-52	_078N	83.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00				550 Feet North of Shanks Rd
78-53	_078N	83.29 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 80 Feet South of South Existing Chevron
78-54	_078N	83.34 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				0.22 Miles North of Shanks Rd
																	New Sign and Post 80 Feet North of North Existing Chevron
																	0.27 Miles North of Shanks Rd
78-55R	_078N	83.87 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	
78-56R	_078N	83.87 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									On the Same Post as 78-55R
78-57	_078N	83.87 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post at Existing Location
78-58	_078N	83.87 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25					4.00				0.15 Miles South of Old CTH U Rd
78-59R	_078N	83.99 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
78-60R	_078N	84.18 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
78-61R	_078N	84.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 78-60R
78-62	_078N	84.30 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 350' North of Existing
78-63	_078N	84.30 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				0.27 Miles North of Old CTH U Rd
78-64R	_078N	84.65 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
78-65R	_078N	84.65 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									On the Same Post as 78-65R
78-66	_078N	84.75 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 450' North of Existing
78-67R	_078S	0.95 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-68R	_078S	0.95 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									On the Same Post as 78-67R
78-69R	_078S	1.31 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
78-70R	_078S	1.31 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 78-69R
78-71	_078S	1.46 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 650' South of Existing
78-72	_078S	1.46 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				0.52 Miles North of Old CTH U Rd

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PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Columbia County STH 78 From CTH DL (North of Merrimac) to IH 39 (South of Portage) Continued STH 78 NB mi at CTH DL is 77.44. STH 78 SB mi at IH 39 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
78-73R	_078S	1.69 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
78-74R	_078S	1.69 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									On the Same Post as 78-74R
78-75	_078S	1.84 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 700' South of Existing
78-76	_078S	1.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25					4.00				0.15 Miles North of Old CTH U Rd
78-77R	_078S	2.29 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	
78-78R	_078S	2.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									On the Same Post as 78-77R
78-79	_078S	2.29 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 250' South of Existing
78-80	_078S	2.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00				500 Feet North of Beich Rd
78-81R	_078S	2.54 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
78-82R	_078S	2.54 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									On the Same Post as 78-81R
78-83	_078S	2.54 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00				New Sign and Post at Existing Location
78-84	_078S	2.54 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00				500 Feet North of Beich Rd
78-85	_078S	2.64 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 80 Feet North of North Existing Chevron
78-86	_078S	2.70 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				0.27 Miles North of Shanks Rd
78-87R	_078S	3.19 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	New Sign and Post 80 Feet South of South Existing Chevron
78-88R	_078S	3.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									0.22 Miles North of Shanks Rd
78-89	_078S	3.30 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				On the Same Post as 78-87R
78-90	_078S	3.30 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				New Sign and Post 500' South of Existing
78-91	_078S	3.39 mi	Right	W1-8	Chevron				1				3.00				0.36 Miles North of Walker Rd
78-92	_078N	3.41 mi	Right	W1-8	Chevron				1				3.00				New Sign and Post
78-93	_078N	3.43 mi	Right	W1-8	Chevron				1				3.00				0.28 Miles North of Walker Rd
78-94R	_078S	3.44 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	New Sign and Post
78-95R	_078S	3.44 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									0.26 Miles North of Walker Rd
78-96	_078S	3.52 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post
78-97	_078S	3.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25					4.00				0.24 Miles North of Walker Rd
78-98R	_078S	3.67 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	New Sign and Post
78-99R	_078S	3.67 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									0.14 Miles North of Walker Rd
78-100	_078S	3.67 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				On the Same Post as 78-98R
78-101	_078S	3.67 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign and Post at Existing Location
78-102R	_078S	4.04 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	100 Feet South of Walker Rd
78-103R	_078S	4.04 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									On the Same Post as 78-102R
78-104	_078S	4.21 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 450' South of Existing
78-105	_078S	4.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				0.55 Miles South of Walker Rd
78-106R	_078S	4.54 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-107R	_078S	4.54 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									On the Same Post as 78-106R
78-108	_078S	4.64 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in					1	6.25				New Sign and Post 400' South of Existing
78-109	_078S	4.64 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				0.24 Miles North of Petra Rd

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Columbia County															638.3000	
STH 78 From CTH DL (North of Merrimac) to IH 39 (South of Portage) Continued															Removing	
STH 78 NB mi at CTH DL is 77.44. STH 78 SB mi at IH 39 is 0.00.															Small	
Sign															Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
78-110R	_078S	5.22 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1	
78-111R	_078S	5.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25								On the Same Post as 78-102R
78-112	_078S	5.28 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00				New Sign and Post 250' South of Existing
78-113	_078S	5.28 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25				4.00				500 Feet North of Messer Rd
78-114	_078S	5.46 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 80 Feet South of South Existing Chevron
78-115R	_078S	7.08 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1	400 Feet South of Messner Rd
78-116R	_078S	7.08 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								On the Same Post as 78-115R
78-117	_078S	7.23 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 600' South of Existing
78-118	_078S	7.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				300 Feet North of Durwards Glen Rd
78-119	_078S	7.35 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1		6.25				
78-120	_078S	7.35 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25				
78-121	_078S	7.53 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1		Replace Sign On Existing Post
78-122R	_078S	7.73 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1	
78-123R	_078S	7.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40								On the Same Post as 78-122R
78-124	_078S	7.90 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 700' South of Existing
78-125	_078S	7.90 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25				0.13 Miles North of Blau Dr
78-126	_078S	8.02 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1		8.00				New Sign and Post
78-127R	_078S	8.04 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in							1	1	50 Feet North of Blau Dr
78-128R	_078S	8.04 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30								On the Same Post as 78-127R
78-129	_078S	8.37 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00				New Sign and Post
78-130	_078S	8.37 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00				0.18 Miles North of CTH DL
Segment Subtotals									16	12	15	366.25	0	33	31	

Columbia and Dane Counties															638.3000	
STH 188 From USH 12 (Sauk City) to STH 113 (Merrimac)															Removing	
STH 188 NB mi at USH 12 is 0.00. STH 188 SB mi at STH 113 is 0.00.															Small	
Sign															Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
188-1	_188N	1.57 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1		Replace Sign on Existing Post
188-2R	_188N	2.52 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1	
188-3R	_188N	3.55 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1	
188-4R	_188N	3.55 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								Attached to the Same Post as Sign 188-3R
188-5	_188N	3.57 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in				1	6.25				New Sign and Post 100' North of Existing
188-6	_188N	3.57 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				550 Feet North of Golf Rd
188-7R	_188N	4.65 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
188-8R	_188N	4.65 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50								Attached to the Same Post as Sign 188-7R
188-9	_188N	4.73 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 350' North of Existing
188-10R	_188N	4.99 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in							1	1	550 Feet South of Price Dr
188-11R	_188N	4.99 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25								Attached to the Same Post as Sign 188-10R

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Columbia and Dane Counties STH 188 From USH 12 (Sauk City) to STH 113 (Merrimac) Continued STH 188 NB mi at USH 12 is 0.00. STH 188 SB mi at STH 113 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
188-12	_188N	5.00 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in	30			1	9.00				New Sign and Post 50' North of Existing 300 Feet South of Fjord Rd	
188-13	_188N	5.00 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in					4.00					
188-14R	_188N	5.84 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in	20						1	1	Attached to the Same Post as Sign 188-14R	
188-15R	_188N	5.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in										
188-16	_188N	5.90 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in	20			1	9.00				New Sign and Post 250' North of Existing 600 Feet South of CTH J	
188-17	_188N	5.90 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in					4.00					
188-18R	_188N	8.66 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in	35						1	1	Attached to the Same Post as Sign 188-18R	
188-19R	_188N	8.66 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in										
188-20M	_188N	8.83 mi	Right	R2-1	Speed Limit (40)	24.00 in	30.00 in	40						1		Move Sign 150' South of Existing Location 600 Feet South of Davis Rd New Sign and Post 950' North of Existing 350 Feet South of Davis Rd	
188-21	_188N	8.86 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in			1	9.00						
188-22	_188N	8.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00					
188-23	_188N	8.94 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00				New Sign and Post At Davis Rd Northeast Corner New Sign and Post At Harmon Rd E Island New Sign and Post 200 Feet North of Harmon Rd E	
188-24	_188N	8.86 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					
188-25	_188N	9.06 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25					
188-26	_188N	9.06 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25					
188-27R	_188N	9.15 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in	25						1	1	Attached to the Same Post as Sign 188-27R	
188-28R	_188N	9.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in										
188-29	_188N	9.18 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in	25			1	9.00				New Sign and Post 100' North of Existing 350 Feet South of CTH V	
188-30	_188N	9.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in					4.00					
188-31R	_188N	9.29 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in	35						1	1	Attached to the Same Post as Sign 188-31R	
188-32R	_188N	9.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in										
188-33	_188N	9.32 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in	30			1	9.00				New Sign and Post 150' North of Existing 350 Feet North of CTH V	
188-34	_188N	9.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in					4.00					
188-35R	_188N	9.52 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in	30						1	1	Attached to the Same Post as Sign 188-35R	
188-36R	_188N	9.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in										
188-37	_188N	9.55 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in	30			1	9.00				New Sign and Post 150' North of Existing 0.27 Miles North of CTH V	
188-38	_188N	9.55 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in					4.00					
188-39	_188N	9.70 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in	40				6.25		1		Replace Sign on Existing Post Remove Sign Only	
188-40R	_188N	9.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in										
188-41R	_188N	9.77 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in	40						1	1	Attached to the Same Post as Sign 188-41R	
188-42R	_188N	9.77 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in										
188-43	_188N	9.80 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 150' North of Existing 350 Feet South of Pleasant View Park Rd (South Junction)	
188-44R	_188N	10.00 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in	40						1	1		
188-45R	_188N	10.00 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in										Attached to the Same Post as Sign 188-44R
188-46M	_188N	10.03 mi	Right	D5-61	Wayside 1/2 Mile	60.00 in	36.00 in							1			
188-47M	_188N	10.03 mi	Right	DB5-69E	Historical Marker	48.00 in	9.00 in										

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Columbia and Dane Counties STH 188 From USH 12 (Sauk City) to STH 113 (Merrimac) Continued STH 188 NB mi at USH 12 is 0.00. STH 188 SB mi at STH 113 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
188-48	_188N	10.05 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 250' North of Existing 100 Feet South of Pleasant View Park Rd (North Junction)
188-49R	_188S	0.08 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1		Attached to the Same Post as Sign 188-49R
188-50R	_188S	0.08 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
188-51R	_188S	0.51 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1		Attached to the Same Post as Sign 188-49R
188-52R	_188S	0.51 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
188-53	_188S	0.60 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25					New Sign and Post 350' South of Existing 300 Feet North of Pleasant View Park Rd (North Junction)
188-54	_188S	0.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25					
188-55R	_188S	0.77 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1		Attached to the Same Post as Sign 188-55R New Sign and Post at Existing Location
188-56R	_188S	0.77 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									
188-57	_188S	0.77 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00					New Sign and Post at Existing Location 550 Feet South of Pleasant View Park Rd (North Junction)
188-58	_188S	0.77 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25				4.00					
188-59	_188S	0.86 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1			Replace Sign on Existing Post
188-60R	_188S	0.91 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1		Attached to the Same Post as Sign 188-60R
188-61R	_188S	0.91 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									
188-62R	_188S	1.17 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in							1	1		Attached to the Same Post as Sign 188-62R New Sign and Post at Existing Location
188-63R	_188S	1.17 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									
188-64	_188S	1.17 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00					New Sign and Post at Existing Location 550 Feet North of CTH V
188-65	_188S	1.17 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20				4.00					
188-66R	_188S	1.32 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1		Attached to the Same Post as Sign 188-66R New Sign and Post at Existing Location
188-67R	_188S	1.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									
188-68	_188S	1.32 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00					New Sign and Post at Existing Location 250 Feet South of CTH V
188-69	_188S	1.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00					
188-70	_188S	1.46 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in				1	6.25					New Sign and Post 300 Feet North of Harmon Rd E
188-71	_188S	1.46 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25					
188-72	_188S	1.58 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					New Sign and Post At Davis Rd Northeast Corner
188-73R	_188S	5.22 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1		Attached to the Same Post as Sign 188-73R
188-74R	_188S	5.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	25									
188-75	_188S	5.29 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00					New Sign and Post 300' South of Existing 150 Feet South of Unke Rd
188-76	_188S	5.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	25				4.00					
188-77R	_188S	5.46 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1		Attached to the Same Post as Sign 188-77R
188-78R	_188S	5.46 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									
188-79	_188S	5.56 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25					New Sign and Post 350' South of Existing 400 Feet South of Fjord Rd
188-80	_188S	5.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					
188-81	_188S	8.20 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1			Replace Sign on Existing Post
188-82	_188S	8.81 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1			Replace Sign on Existing Post
188-83	_188S	9.54 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1	6.25					New Sign and Post 0.59 Miles South of Inama Rd
Segment Subtotals										6	3	13	259.75	2	24	19	

PROJECT NO: 1000-20-80				HWY: VARIOUS		COUNTY: VARIOUS – SW REGION		MISCELLANEOUS QUANTITIES		SHEET NO:		E
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3

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

FILE NAME : _____

PLOT DATE : _____

PLOT BY : _____

PLOT NAME : _____

ORG DATE : _____

ORIGINATOR : DIST _

PLOT SCALE : 1:1

Sauk County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 113 From STH 78 (West of Merrimac) to STH 123 (Baraboo)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 113 NB mi at STH 78 is 26.38. STH 113 SB mi at STH 123 is 0.51.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign										14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines		Each	Each	Each	SF	Each	Each	Each	Notes
113-1R	_113N	28.78 mi	Right	W1-5-L	Left Winding Road	36.00 in	36.00 in								1	1	
113-2R	_113N	28.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-1R
113-3	_113N	28.84 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in					1	6.25				New Sign and Post 200' North of Existing
113-4	_113N	28.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				250 Feet South of South Lake Rd
113-5	_113N	28.94 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
113-6	_113N	29.02 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				300 Feet North of South Lake Rd
113-7	_113N	29.18 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
113-8	_113N	29.30 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				0.14 Miles North of South Lake Rd
113-9	_113N	29.30 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign and Post
113-10R	_113N	29.74 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	0.42 Miles North of South Lake Rd
113-11R	_113N	29.74 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									Attached to the Same Post as 113-10R
113-12R	_113N	30.18 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
113-13R	_113N	30.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35					6.25				Attached to the Same Post as 113-12R
113-14	_113N	30.18 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		2.25				New Sign and Post at Existing Location
113-15	_113N	30.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									0.63 Miles South of CTH DL
113-16	_113N	30.36 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
113-17R	_113N	30.59 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1		0.48 Miles South of CTH DL
113-18R	_113N	30.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									Leave Existing Post
113-19	_113N	30.59 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in						6.25				Attached to the Same Post as 113-17R
113-20	_113N	30.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				New Sign on Existing Post
113-21R	_113N	30.77 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	New Sign on Existing Post
113-22R	_113N	30.96 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1		Leave Existing Post
113-23R	_113N	30.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									Attached to the Same Post as 113-22R
113-24	_113N	30.96 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-25	_113N	30.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign on Existing Post
113-26R	_113N	31.44 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
113-27R	_113N	31.44 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									Attached to the Same Post as 113-26R
113-28R	_113N	31.77 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
113-29R	_113N	31.77 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as 113-28R
113-30	_113N	31.81 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 300' North of Existing
113-31	_113N	31.81 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
113-32R	_113N	32.07 mi	Right	W1-5-R	Right Winding Road	36.00 in	36.00 in								1		Leave Existing Post
113-33R	_113N	32.07 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-32R
113-34	_113N	32.07 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-35	_113N	32.07 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				New Sign on Existing Post
113-36	_113N	32.20 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
113-37R	_113N	32.51 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
113-38R	_113N	32.51 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-37R
113-39	_113N	33.72 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post

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Sauk County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 113 From STH 78 (West of Merrimac) to STH 123 (Baraboo) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 113 NB mi at STH 78 is 26.38. STH 113 SB mi at STH 123 is 0.51.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign								Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	Notes
113-40R	_113S	1.40 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in								1	1	
113-41R	_113S	1.40 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	30.00 in	25									Attached to the Same Post as 113-40R
113-42	_113S	1.52 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
113-42	_113S	1.60 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
113-43	_113S	1.86 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 100' South of Existing
113-44	_113S	1.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	30.00 in	30					4.00				
113-45R	_113S	1.88 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	Attached to the Same Post as 113-45R
113-46R	_113S	1.88 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	30.00 in	25									
113-47R	_113S	2.62 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1		Leave Existing Post
113-48R	_113S	2.62 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-47R
113-49	_113S	2.62 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-50	_113S	2.62 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				New Sign on Existing Post
113-51R	_113S	2.79 mi	Right	W1-5-L	Left Winding Road	36.00 in	36.00 in								1		Leave Existing Post
113-52R	_113S	2.79 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-51R
113-53	_113S	2.79 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-54	_113S	2.79 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				New Sign on Existing Post
113-55	_113S	3.08 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
113-56R	_113S	3.29 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1		Leave Existing Post
113-57R	_113S	3.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-56R
113-58	_113S	3.29 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-59	_113S	3.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign on Existing Post
113-60R	_113S	3.52 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as 113-60R
113-61R	_113S	3.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									
113-62	_113S	3.70 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				New Sign and Post
113-63R	_113S	4.14 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1		650 Feet North of Wildw ood Rd
113-64R	_113S	4.14 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									Leave Existing Post
113-65	_113S	4.14 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in						6.25				Attached to the Same Post as 113-63R
113-66	_113S	4.14 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				New Sign on Existing Post
113-67R	_113S	4.48 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1		Leave Existing Post
113-68R	_113S	4.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									Attached to the Same Post as 113-67R
113-69	_113S	4.48 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-70	_113S	4.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				New Sign on Existing Post
113-71R	_113S	4.60 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
113-72R	_113S	4.82 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1		Leave Existing Post
113-73R	_113S	4.82 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-73R
113-74	_113S	4.82 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-75	_113S	4.82 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign on Existing Post
113-76R	_113S	5.02 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
113-77R	_113S	5.24 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	0.48 Miles South of CTH DL
113-78R	_113S	5.24 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									Attached to the Same Post as 113-77R

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PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

3

Sauk County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
STH 113 From STH 78 (West of Merrimac) to STH 123 (Baraboo) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 113 NB mi at STH 78 is 26.38. STH 113 SB mi at STH 123 is 0.51.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number								Lines		Each	Each	Each	SF	Each	Each	Each	
113-79R	_113S	5.69 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1		Leave Existing Post
113-80R	_113S	5.69 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as 113-79R
113-81	_113S	5.69 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-82R	_113S	5.89 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in								1		Leave Existing Post
113-83R	_113S	5.89 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as 113-82R
113-84	_113S	5.89 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in						6.25				New Sign on Existing Post
113-85	_113S	5.89 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign on Existing Post
113-86	_113S	5.98 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
113-87	_113S	6.09 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in					1	6.25				0.52 Miles North of South Lake Rd
113-88	_113S	6.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				New Sign and Post
																	0.40 Miles North of South Lake Rd
113-89	_113S	6.19 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
113-90	_113S	6.44 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				0.30 Miles North of South Lake Rd
113-91R	_113S	6.50 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	New Sign and Post
113-92R	_113S	6.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									250 Feet North of South Lake Rd
																	Attached to the Same Post as 113-91R
Segment Subtotals										9	3	3	257.00	0	29	13	

Sauk County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
STH 136 From STH 33 (East of Reedsburg) to STH 154 (Rock Springs)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 136 EB mi at STH 33 is 0.00. STH 136 WB mi at STH 154 is 7.90.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number								Lines		Each	Each	Each	SF	Each	Each	Each	
136-1R	_136E	0.35 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
136-2R	_136E	1.77 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
136-3R	_136E	2.40 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
136-4R	_136E	2.40 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as 136-3R
136-5	_136E	2.47 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 350' East of Existing
136-6	_136E	2.47 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.14 Miles West of Mile Rd
136-7R	_136E	2.75 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
136-8R	_136E	2.75 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as 136-7R
136-9R	_136E	4.13 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
136-10R	_136E	4.13 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	30									Attached to the Same Post as 136-9R
136-11	_136E	4.19 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 200' East of Existing
136-12	_136E	4.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				0.38 Miles East of Ableman Rd
136-13	_136E	4.23 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
136-14R	_136E	4.33 mi	Right	W1-5-R	Right Winding Road	36.00 in	36.00 in								1	1	
136-15	_136E	4.48 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in		1				6.25				New Sign and Post
136-16M	_136E	4.52 mi	Right	R2-1	Speed Limit (35)	24.00 in	30.00 in	35						1			0.61 Miles West of STH 154
136-17R	_136W	8.09 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in								1	1	Move Existing Sign and Post 150' East
																	0.57 Miles West of STH 54

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Sauk County										638.3000									
STH 136 From STH 33 (East of Reedsburg) to STH 154 (Rock Springs)										634.0614		634.0616		634.0618		637.2230	638.2102	638.2602	Removing
STH 136 EB mi at STH 33 is 0.00. STH 136 WB mi at STH 154 is 7.90.										Posts Wood		Posts Wood		Posts Wood		Signs	Moving	Removing	Small
Sign	Number	Route	Marker	Position	Code	Description	Width	Height	Order	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Type II	Type II	Sign			
									Lines	14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports			
										Each	Each	Each	SF	Each	Each	Each			
																Notes			
136-18	_136W	8.30 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1			6.25			New Sign and Post			
136-19	_136W	8.71 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1	0.41 Miles West of STH 54			
																Replace Sign on Existing Post			
136-20R	_136W	8.79 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1				
136-21R	_136W	8.79 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								1			
																Attached to the Same Post as 136-20R			
136-22	_136W	8.82 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1			6.25						
136-23	_136W	8.82 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25			New Sign and Post 150' West of Existing			
																0.35 Miles East of Ableman Rd			
136-24R	_136W	9.89 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1				
136-25R	_136W	9.89 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								1			
																Attached to the Same Post as 136-24R			
136-26R	_136W	10.13 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1				
136-27R	_136W	10.13 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								1			
																Attached to the Same Post as 136-26R			
136-28	_136W	10.21 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25						
136-29	_136W	10.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25			New Sign and Post 400' West of Existing			
																0.15 Miles East of Mile Rd			
136-30R	_136W	10.56 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1				
136-31R	_136W	12.15 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1				
															1				
Segment Subtotals										2	3	1	67.00	1	14	12			

Juneau and Sauk Counties										638.3000							
STH 33 From STH 80 (Union Center) to STH 23 (Reedsburg)										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	
STH 33 EB mi at STH 80 is 59.84. STH 33 WB mi at STH 23 is 85.00.										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	Sign	
Number								Lines	14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	Supports	Notes
									Each	Each	Each	SF	Each	Each	Each	Each	
33-1R	_033E	60.24 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 33-2R
33-2R	_033E	60.61 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
33-3R	_033E	60.61 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									
33-4R	_033E	61.83 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
33-5R	_033E	62.20 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 33-5R
33-6R	_033E	62.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									
33-7R	_033E	62.83 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
33-8	_033E	63.49 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in										Leave Existing Remove Sign Only New Speed Advisory on Existing Post
33-9R	_033E	63.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15							1		
33-10	_033E	63.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	10				4.00					
33-11	_033E	63.54 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00			1		Replace Sign on Existing Post
33-12R	_033E	63.72 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
33-13R	_033E	64.10 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
33-14R	_033E	64.66 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
33-15R	_033E	70.81 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	

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Juneau and Sauk Counties										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 33 From STH 80 (Union Center) to STH 23 (Reedsburg) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 33 EB mi at STH 80 is 59.84. STH 33 WB mi at STH 23 is 85.00.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
Number								Lines		Each	Each	Each	SF	Each	Each	Each	
33-16	_033E	71.62 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 50' West of Existing At La Valle St Southeast Corner Remove Sign From Light Pole
33-17R	_033E	71.63 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1		
33-18R	_033E	72.30 mi	Left	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
33-19R	_033E	72.85 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
33-20R	_033E	73.95 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
33-21R	_033E	77.17 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
33-22R	_033W	85.99 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
33-23R	_033W	89.12 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1		Remove Sign From Behind No Passing Zone Sign
33-24R	_033W	89.60 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
33-25	_033E	89.66 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 250' West of Existing 600 Feet East of Schuette Rd
33-26R	_033W	90.85 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
33-27R	_033W	91.23 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
33-28	_033W	91.30 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 250' West of Existing 0.39 Miles East of Gudenschager Rd
33-29R	_033W	92.31 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
33-30R	_033W	98.27 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
33-31R	_033W	99.57 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
33-32R	_033W	99.78 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in								1		Remove From Utility Pole
33-33R	_033W	99.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15									Attached to the Same Utility Pole as 33-32R
33-34	_033W	99.81 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 150' West of Existing 350 Feet East of North East St
33-35	_033W	99.81 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	10					4.00				
33-36	_033W	99.95 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
33-37R	_033W	100.87 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1		Remove Sign From Behind No Passing Zone Sign
33-38R	_033W	101.80 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
33-39R	_033W	102.21 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
33-40R	_033W	102.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 33-39R
33-41R	_033W	102.77 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1		Remove Sign From Behind No Passing Zone Sign
33-42R	_033W	103.15 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
Segment Subtotals										2	1	1	51.75	0	31	23	

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Monroe County										638.3000							
STH 71 From STH 16 (Sparta) to STH 80 (Elroy)										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	
STH 71 EB mi at STH 16 is 6.30. STH 71 WB mi at STH 80 is 0.00.										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Type II	Type II	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
								Lines		Each	Each	Each	SF	Each	Each	Each	
71-1	_071E	13.46 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 550 Feet North of Karlstad Rd
71-2R	_071E	18.19 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 71-2R
71-3R	_071E	18.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25									
71-4	_071E	22.22 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 600 Feet South of Lignite Rd
71-5	_071E	25.09 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 0.38 Miles North of Linden Ave
71-6	_071E	26.52 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 0.26 Miles North of Livingston Rd
71-7R	_071E	27.07 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1		Remove Sign From Behind Existing No Passing Zone Sign
71-8R	_071E	29.85 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	New Sign and Post 250' South of Existing 0.31 Miles North of Spring St, Kendall
71-9R	_071E	33.04 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
71-10	_071E	33.04 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1			6.25				
71-11	_071W	6.40 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in						6.25		1		Replace Sign on Existing Post
71-12R	_071W	9.60 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	New Sign and Post 600' North of Existing 200 Feet South of CTH V
71-13	_071W	9.73 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				
71-14R	_071W	12.19 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	Replace Sign on Existing Post
71-15	_071W	21.04 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in						9.00		1		
71-16	_071W	28.38 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				
Segment Subtotals										7	0	0	59.00	0	8	5	

Richland and Vernon Counties										638.3000							
STH 80 From CTH D North Junction (Rockbridge) to STH 33 (Hillsboro)										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	
STH 80 NB mi at CTH D is 74.95. STH 80 SB mi at STH 33 is 44.23.										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign									Order	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
										Each	Each	Each	SF	Each	Each	Each	
80-1-1R	_080N	77.90 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
80-1-2R	_080N	77.90 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as Sign 80-1-1R
80-1-3	_080N	77.96 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 200 Feet North of Existing
80-1-4	_080N	77.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				750 Feet South of CTH C
80-1-5R	_080N	78.41 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
80-1-6R	_080N	78.41 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as Sign 80-1-5R
80-1-7R	_080N	78.78 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
80-1-8R	_080N	90.18 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	
80-1-9R	_080N	90.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									Attached to the Same Post as Sign 80-1-8R
80-1-10	_080N	90.22 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 150' North of Existing
80-1-11	_080N	90.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00				50 Feet North of Prairie Ave, Hillsboro
80-1-12	_080N	90.26 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

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Richland and Vernon Counties										638.3000							
STH 80 From CTH D North Junction (Rockbridge) to STH 33 (Hillsboro) Continued										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	Notes
STH 80 NB mi at CTH D is 74.95. STH 80 SB mi at STH 33 is 44.23.										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Type II	Type II	Sign		
Number								Lines	14 Foot	16 Foot	18 Foot	Reflective F	Each	Each	Each	Supports	
									Each	Each	Each	SF				Each	
80-1-13R	_080S	44.36 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
80-1-14R	_080S	44.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									
80-1-15	_080S	44.41 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00					New Sign and Post 150' South of Existing 50 Feet South of Lake St, Hillsboro
80-1-16	_080S	44.41 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20				4.00					
80-1-17	_080S	44.43 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00			1		Replace Sign on Existing Post
80-1-18	_080S	51.43 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 0.30 Miles North of Pliner Rd Replace Sign on Existing Post
80-1-19	_080S	55.69 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in					6.25			1		
80-1-20R	_080N	55.82 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as Sign 80-1-20R
80-1-21R	_080N	55.82 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									
80-1-22R	_080N	56.21 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as Sign 80-1-22R
80-1-23R	_080N	56.21 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									
Segment Subtotals										0	2	2	63.00	0	10	7	

Sauk County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
STH 130 From Richland County (Bear Valley) to STH 154 (West of Loganville)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 130 NB mi at Sauk County is 22.49. STH 130 SB mi at STH 154 is 0.00.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	14 Foot Each	16 Foot Each	18 Foot Each	Reflective F SF	Type II Each	Type II Each	Type II Each	Small Sign Supports Each	
130-1R	_130N	22.52 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 130-1R	
130-2R	_130N	22.52 mi	Right	W8-8	Rough Road	30.00 in	30.00 in										
130-3R	_130N	23.38 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1		
130-4	_130N	23.52 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in				1	6.25				New Sign and Post 650' North of Existing 450 Feet South of CTH N (South Junction)	
130-5	_130N	23.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					
130-6	_130N	23.61 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					
130-7	_130N	23.68 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00				New Sign and Post At CTH N (South Junction) Northwest Corner New Sign and Post At CTH N (North Junction) Northeast Corner	
130-8R	_130N	23.73 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1		
130-9R	_130N	23.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
130-10	_130N	23.73 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25				Attached to the Same Post as Sign 130-8R New Sign and Post at Existing Location 350 Feet North of CTH N (North Junction)	
130-11	_130N	23.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25					
130-12	_130N	23.85 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00		1	1		
130-13R	_130N	23.96 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1	Attached to the Same Post as Sign 130-13R	
130-14R	_130N	23.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
130-15R	_130N	24.28 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1		
130-16R	_130N	25.15 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1		
130-17R	_130N	25.83 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	Attached to the Same Post as Sign 130-17R	
130-18R	_130N	25.83 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
130-19	_130N	25.93 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25					
130-20	_130N	25.93 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25				New Sign and Post 450' North of Existing 0.23 Miles South of Croal Rd	

Sauk County STH 130 From Richland County (Bear Valley) to STH 154 (West of Loganville) Continued STH 130 NB mi at Sauk County is 22.49. STH 130 SB mi at STH 154 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									Notes
130-21	_130N	26.04	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in				1	8.00					New Sign and Post 600 Feet South of Croal Rd
130-22R	_130N	27.35 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1		1	Attached to the Same Post as Sign 130-22R
130-23R	_130N	27.35 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									
130-24	_130N	27.42 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 300' North of Existing 0.27 Miles South of Hill Rd
130-25	_130N	27.60 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 600 Feet South of Croal Rd
130-26	_130N	27.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25					
130-27R	_130N	27.69 mi	Right	W1-5-R	Right Winding Road	36.00 in	36.00 in							1		1	Attached to the Same Post as Sign 130-27R
130-28R	_130N	27.69 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
130-29	_130N	28.00 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 0.31 Miles North of Hill Rd New Sign and Post 0.65 Miles North of Hill Rd
130-30	_130N	28.34 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25					
130-31	_130N	28.34 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					
130-32R	_130N	28.49 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in							1		1	
130-33R	_130N	28.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 130-32R
130-34	_130N	28.56 mi	Right	W1-5-L	Left Winding Road	36.00 in	36.00 in			1		6.25					New Sign and Post 350' North of Existing 0.88 Miles North of Hill Rd
130-35	_130N	28.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					
130-36M	_130N	28.65 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1			Move to Center of Approaching Lane 0.96 Miles North of Hill Rd Move to Center of Approaching Lane 1.04 Miles North of Hill Rd
130-37M	_130N	28.73 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1			
130-38	_130N	28.95 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					New Sign and Post 0.82 Miles South of Sandusky Rd
130-39R	_130N	29.19 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
130-40R	_130N	29.31 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
130-41R	_130N	29.31 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									
130-42	_130N	29.45 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 650' North of Existing 0.32 Miles South of Sandusky Rd
130-43	_130N	29.45 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					
130-44	_130N	29.60 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25			1	1	Replace Signs and Post at Existing Location 0.15 Miles South of Sandusky Rd
130-45	_130N	29.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25					
130-46R	_130S	0.74 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 130-46R
130-47R	_130S	0.74 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									
130-48	_130S	0.78 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 200' South of Existing 750 Feet North of Sandusky Rd
130-49	_130S	0.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					
130-50M	_130S	0.79 mi	Right	W7-1	Hill Symbol	36.00 in	36.00 in							1			Move 150' South 600 Feet North of Sandusky Rd
130-51R	_130S	1.03 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
130-52R	_130S	1.03 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as Sign 130-51R
130-53R	_130S	1.35 mi	Right	W1-5-R	Right Winding Road	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 130-53R
130-54R	_130S	1.35 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	30									
130-55	_130S	1.36 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in				1	9.00					New Sign and Post 50' South of Existing 0.42 Miles South of Sandusky Rd
130-56	_130S	1.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in					4.00					

Sauk County STH 130 From Richland County (Bear Valley) to STH 154 (West of Loganville) Continued STH 130 NB mi at Sauk County is 22.49. STH 130 SB mi at STH 154 is 0.00.										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
Sign										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
Number	Route	Marker	Position	Code	Description	Width	Height	Order		4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	Small
								Lines		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	Sign
										Each	Each	Each	SF	Each	Each	Each	Supports
130-57R	_130S	1.47 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
130-58R	_130S	1.61 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in								1	1	
130-59R	_130S	1.61 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
130-60	_130N	1.69 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				Attached to the Same Post as Sign 130-58R New Sign and Post 0.77 Miles South of Sandusky Rd
130-61	_130N	1.78 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in					1	6.25				
130-62	_130N	1.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign and Post 0.86 Miles South of Sandusky Rd
130-63M	_130S	2.01 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1			Move to Center of Approaching Lane 1.02 Miles North of Hill Rd
130-64R	_130S	2.09 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in								1	1	
130-65R	_130S	2.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 130-64R
130-66	_130N	2.23 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in				1		6.25				
130-67	_130N	2.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign and Post 0.78 Miles North of Hill Rd
130-68	_130S	2.67 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in					1	6.25				
130-69	_130S	2.67 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign and Post 0.34 Miles North of Hill Rd
130-70	_130S	2.80 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 0.21 Miles North of Hill Rd
130-71	_130S	3.03 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25		1	1	
130-72	_130S	3.03 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				Replace Signs and Post at Existing Location 200 Feet South of Hill Rd
130-73R	_130S	4.37 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
130-74R	_130S	4.37 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 130-73R
130-75	_130S	4.50 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				
130-76	_130S	4.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign and Post 550' South of Existing 200 Feet North of Croal Rd
130-77	_130S	4.59 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 250 Feet South of Croal Rd
130-78R	_130S	4.97 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
130-79R	_130S	5.99 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
130-80R	_130S	6.30 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
130-81R	_130S	6.30 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 130-80R New Sign and Post 0.58 Miles North of CTH N (North Junction)
130-82	_130S	6.43 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				
130-83R	_130S	6.66 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
130-84R	_130S	6.66 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 130-83R
130-85	_130S	6.68 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				
130-86	_130S	6.68 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign and Post 100' South of Existing 0.31 Miles North of CTH N (North Junction)
130-87	_130S	6.80 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in					1	6.25				
130-88	_130S	6.80 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				New Sign and Post 350' North of Existing 0.21 Miles North of CTH N (North Junction)
130-89R	_130S	6.86 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
130-90R	_130S	6.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as Sign 130-81R
130-91	_130S	7.09 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post
130-92R	_130S	7.82 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
130-93	_130S	7.96 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 600' South of Existing 0.89 Miles South of CTH N (South Junction)
Segment Subtotals										10	13	7	246.00	4	28	28	

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Sauk County															
STH 60 From USH 14 (Spring Green) to CTH B (West of Sauk City)															
STH 60 EB mi at USH 14 is 45.08. STH 60 WB mi at CTH B is 58.70.															
Sign								Order Lines	634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000
Number	Route	Marker	Position	Code	Description	Width	Height		Posts Wood 4 x 6 - Inch 14 Foot Each	Posts Wood 4 x 6 - Inch 16 Foot Each	Posts Wood 4 x 6 - Inch 18 Foot Each	Signs Type II Reflective F SF	Moving Signs Type II Each	Removing Signs Type II Each	Removing Small Sign Supports Each
60-1R	_060E	47.56 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1
60-2R	_060E	48.24 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1
60-3	_060E	48.31 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25			
60-4	_060E	48.31 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			
60-5R	_060E	48.77 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1	1
60-6	_060E	48.80 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00			
60-7	_060E	48.89 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in				1	9.00			
60-8	_060E	48.89 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00			
60-9R	_060E	48.93 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in							1	1
60-10R	_060E	48.93 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30							
60-11R	_060E	50.37 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1
60-12R	_060E	50.82 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	
60-13R	_060E	50.82 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40							
60-14	_060E	50.93 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1		6.25			
60-15	_060E	50.93 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25			
60-16	_060E	51.03 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00			
60-17R	_060E	51.52 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1
60-18R	_060E	51.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40							
60-19	_060E	51.60 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								
60-20	_060E	51.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45							
60-21R	_060E	52.23 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1
60-22R	_060E	53.10 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in							1	1
60-23R	_060E	53.10 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45							
60-24R	_060E	53.78 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	
60-25R	_060E	53.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45							
60-26	_060E	53.87 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25			
60-27	_060E	53.87 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25			
60-28R	_060E	54.11 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in							1	1
60-29R	_060E	54.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40							
60-30R	_060E	55.59 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1
60-31	_060E	57.72 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in		1			6.25			
60-32	_060W	58.72 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25			
60-33	_060W	60.01 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in		1			6.25			
60-34R	_060W	61.80 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1

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Sauk County STH 60 From USH 14 (Spring Green) to CTH B (West of Sauk City) Continued STH 60 EB mi at USH 14 is 45.08. STH 60 WB mi at CTH B is 58.70.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
60-35R	_060W	63.31 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
60-36R	_060W	63.31 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as 60-35R
60-37	_060W	63.57 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 700' East of Existing
60-38	_060W	63.57 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.62 Miles East of McDonalds Subdivision Rd
60-39R	_060W	63.70 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
60-40R	_060W	63.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as 60-39R
60-41R	_060W	64.06 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
60-42R	_060W	64.06 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as 60-41R
60-43R	_060W	64.71 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
60-44R	_060W	65.56 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
60-45R	_060W	65.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as 60-44R
60-46	_060W	65.63 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 300' West of Existing
60-47	_060W	65.63 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				0.43 Miles West of Badger Rd
60-48R	_060W	66.49 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
60-49R	_060W	66.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as 60-48R
60-50	_060W	66.59 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 400' West of Existing
60-51	_060W	66.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.31 Miles East of Williams Rd
60-52R	_060W	66.99 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1		Remove Sign from Behind No Passing Zone Sign
60-53R	_060W	68.18 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
60-54R	_060W	68.18 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									Attached to the Same Post as 60-53R
60-55	_060W	68.49 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in			1			6.25		1	1	New Sign and Post at Existing Location
60-56	_060W	68.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				250 Feet West of CTH C
60-57	_060W	68.77 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in					1	9.00		1	1	New Sign and Post at Existing Location
60-58	_060W	68.77 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				0.36 Miles West of CTH C
60-59	_060W	68.97 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1			6.25		1	1	New Sign and Post at Existing Location
60-60	_060W	68.97 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				0.53 Miles West of CTH C
60-61R	_060W	69.12 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
60-62	_060W	69.15 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 150' West of Existing
60-63R	_060W	69.34 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	0.67 Miles West of CTH C
60-64R	_060W	69.74 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
Segment Subtotals										6	8	2	136.75	0	27	24	

3

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

3

Iowa County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 80 From USH 18 (Cobb) to STH 133 (East of Muscoda)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 80 NB mi at USH 18 is 31.72. STH 80 SB mi at STH 133 is 85.42.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	Notes
Number								Lines		Each	Each	Each	SF	Each	Each	Each	
80-1R	_080N	32.39 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
80-2R	_080N	32.74 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
80-3	_080N	33.98 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 0.29 Miles North of Stephens Rd
80-4	_080N	34.87 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 250 Feet South of CTH BH
80-5	_080N	36.33 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 0.14 Miles South of Clark Rd
80-6R	_080N	36.66 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
80-7R	_080N	36.66 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									On Same Post as 80-6R
80-8R	_080N	37.65 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
80-9	_080N	37.78 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 650' North of Existing 0.24 Miles South of Tow er Ln
80-10	_080N	38.26 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 0.15 Miles South of Kennedy St (Highland)
80-11R	_080N	39.51 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
80-12	_080N	39.55 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 200' North of Existing 450 Feet North of Varsity Ln
80-13R	_080N	39.85 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1		Remove Sign From Behind No Passing Zone Sign
80-14R	_080N	40.77 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
80-15	_080N	41.19 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 0.15 Miles North of Camp Rd
80-16R	_080N	43.03 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
80-17R	_080N	43.79 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
80-18	_080N	44.07 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 450 Feet South of CTH P (North Junction)
80-19E	_080N	45.20 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in										Leave This Existing Sign
80-20R	_080N	45.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40							1		Remove 40 Advisory Only
80-21	_080N	45.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				Replace With 35 Advisory
80-22	_080N	45.26 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 0.68 Miles North of Aide Rd
80-23	_080N	45.28 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 0.70 Miles North of Aide Rd
80-24R	_080N	45.30 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
80-25	_080N	45.30 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 0.72 Miles North of Aide Rd
80-26	_080N	45.32 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 0.74 Miles North of Aide Rd
80-27	_080N	45.34 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 0.76 Miles North of Aide Rd
80-28	_080N	45.36 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 0.78 Miles North of Aide Rd
80-29E	_080S	87.96 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in										Leave This Existing Sign
80-30R	_080S	87.96 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50							1		Remove 50 Advisory Only

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

3

Iowa County										638.3000							
STH 80 From USH 18 (Cobb) to STH 133 (East of Muscoda) Continued										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing	
STH 80 NB mi at USH 18 is 31.72. STH 80 SB mi at STH 133 is 85.42.										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small	
Sign									Order	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports	Notes
80-31E	_080N	45.20 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in										Leave This Existing Sign
80-32R	_080N	45.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45							1		Remove 45 Advisory Only
80-33	_080N	45.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				Replace With 35 Advisory
80-34	_080N	45.26 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
80-35	_080N	45.28 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				0.78 Miles North of Aide Rd
80-36	_080N	45.30 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
80-37	_080N	45.32 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				0.74 Miles North of Aide Rd
80-38	_080N	45.34 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post
80-39	_080N	45.36 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00				0.70 Miles North of Aide Rd
80-40R	_080S	91.33 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	New Sign and Post
80-41	_080S	92.33 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				0.68 Miles North of Aide Rd
80-42R	_080S	93.46 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	New Sign and Post
80-43R	_080S	94.26 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	0.21 Miles South of CTH P (South Junction)
80-44R	_080S	94.80 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
80-45R	_080S	97.44 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
80-46R	_080S	97.44 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									On Same Post as 80-45R
80-47	_080S	97.55 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post
80-48	_080S	97.55 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				0.13 Miles North of CTH Q
80-49	_080S	99.16 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post
80-50	_080S	99.41 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				300 Feet North of Bills Rd
80-51R	_080S	101.45 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	New Sign and Post
80-52R	_080S	101.82 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	0.19 Miles South of Bills Rd
80-53	_080S	101.89 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 350' South of Existing
Segment Subtotals										19	6	0	124.00	0	20	16	100 Feet North of Willow Springs Rd

PROJECT NO: 1000-20-80	HWY: VARIOUS	COUNTY: VARIOUS – SW REGION	MISCELLANEOUS QUANTITIES	SHEET NO:
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Grant County															638.3000						
STH 133 From CTH T (Blue River) to STH 80 (Muscoda)															634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Removing
STH 133 NB mi at CTH T is 57.34. STH 133 SB mi at STH 80 is 8.16.															Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Small
Sign								Order	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign						
Number	Route	Marker	Position	Code	Description	Width	Height	Lines	14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports						
									Each	Each	Each	SF	Each	Each	Each						
133-1R	_133N	58.08 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1						
133-2R	_133N	59.26 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1						
133-3R	_133S	12.09 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1						
133-4R	_133S	12.36 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1						
133-5R	_133S	14.31 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1						
Segment Subtotals									0	0	0	0.00	0	5	5						

Grant County										634.0614						634.0616	634.0618	637.2230	638.2102	638.2602	638.3000
STH 35 From STH 81 (West of Lancaster) to USH 18 (Patch Grove)										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing					
STH 35 NB mi at STH 81 is 8.16. STH 35 SB mi at USH 18 is 68.54.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	Small				
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	Supports				
Number								Lines		Each	Each	Each	SF	Each	Each	Each	Each				
35-1	_035N	14.75 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in						6.25		1		Replace Sign on Existing Post				
35-2R	_035N	15.06 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	On Same Post as 35-2R				
35-3R	_035N	15.06 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45													
35-4R	_035N	17.62 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1					
35-6R	_035N	18.24 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1					
35-7R	_035N	19.81 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1					
35-8R	_035N	20.05 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1					
35-9	_035N	20.11 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 250' North of Existing 0.43 Miles South of Lancaster St, Patch Grove Replace Sign on Existing Post				
35-10	_035S	69.52 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in						6.25		1						
35-11R	_035S	69.89 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1					
35-12R	_035S	71.24 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	New Sign and Post 200' South of Existing Across from Breuer Rd				
35-13R	_035S	71.99 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1					
35-14	_035S	72.04 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25								
35-15M	_035S	72.08 mi	Right	I55-56	Adopt a Highway	30.00 in	36.00 in	River Ridge Middle School						1			Move Sign 200' South of Existing Location 200' South of Breuer Rd				
35-16R	_035S	74.97 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1					
Segment Subtotals										2	0	0	25.00	1	11	9					

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Lafayette County															638.3000	
STH 11 From STH 80 (North of Hazel Green) to CTH O (Shullsburg)															Removing	
STH 11 EB mi at STH 80 is 12.65. STH 11 WB mi at CTH O is 74.65.															Small	
Sign									634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Order	Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Supports	
								Lines	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
									14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Each	
									Each	Each	Each	SF	Each	Each	Each	
															Notes	
11-1	_011E	15.05 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1	Replace Sign On Existing Post	
11-2	_011E	15.53 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1			6.25		1	1	Replace Sign and Post at Existing Location
11-3R	_011E	17.23 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1	Attached to the Same Post as Sign 11-3R
11-4R	_011E	17.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50								
11-5	_011E	17.39 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 650' East of Existing 500 Feet East of Bennet Rd New Sign and Post 0.89 Miles West of Palfery Rd
11-6	_011E	18.48 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				
11-7R	_011E	20.83 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
11-8R	_011W	76.45 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
11-9R	_011W	79.66 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1	Attached to the Same Post as Sign 11-10R
11-10R	_011W	79.66 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50								
11-11R	_011W	81.08 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1	Attached to the Same Post as Sign 11-13 Replace Sign On Existing Post
11-12R	_011W	82.71 mi	Right	M1-6	State Route Marker (11)	24.00 in	24.00 in	11								
11-13	_011W	82.71 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1		
Segment Subtotals									2	1	0	34.75	0	8	6	

Lafayette and Iowa Counties															638.3000	
STH 23 From STH 11 (South of Darlington) to CTH ZZ (North of Dodgeville)															Removing	
STH 23 EB mi at STH 11 is 0.00. STH 23 WB mi at CTH ZZ is 55.36.															Small	
Sign									634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Sign	
Number	Route	Marker	Position	Code	Description	Width	Height	Order	Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Supports	
								Lines	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Type II	Type II	Each	
									14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Each	
									Each	Each	Each	SF	Each	Each	Each	
															Notes	
23-1R	_023E	3.93 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1	New Sign and Post 600 Feet West of Olson Rd
23-2R	_023E	8.16 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
23-3R	_023E	8.52 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
23-4	_023E	14.30 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25				
23-5R	_023E	17.49 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1	Replace Sign on Existing Post
23-6	_023E	22.80 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1		
23-7R	_023W	65.57 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1	
23-8R	_023W	65.57 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15								Attached to the Same Post as Sign 23-7R
23-9	_023W	65.62 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00				New Signs and Post 250' West of Existing 50 Feet East of Vine St, Mineral Point
23-10	_023W	65.62 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15				4.00				
23-11R	_023W	68.75 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	New Sign and Post 250' West of Existing 0.32 Miles East of Rock Branch Rd
23-12	_023W	68.80 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				
23-13	_023W	74.21 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 750 Feet East of Reilly Rd
23-14R	_023W	77.90 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
23-15R	_023W	78.33 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
23-16	_023W	82.66 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in					6.25		1		Replace Sign on Existing Post
Segment Subtotals									2	1	1	46.00	0	10	8	

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Lafayette County STH 81 From STH 23 (Darlington) to Green County STH 81 EB mi at STH 23 is 40.52. STH 81 WB mi at Lafayette County is 35.85.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
81-1R	_081E	41.32 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 81-1R
81-2R	_081E	41.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									
81-3R	_081E	42.26 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-4R	_081E	43.42 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	New Sign and Post 350' East of Existing 0.43 Miles West of Otter Creek Rd
81-5	_081E	43.49 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1				6.25				
81-6R	_081E	43.71 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-7R	_081E	44.46 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-8R	_081E	44.99 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
81-9R	_081E	45.59 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-10R	_081E	46.35 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 81-10R
81-11R	_081E	46.35 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									
81-12R	_081E	47.63 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-13R	_081E	47.63 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 81-12R New Signs and Post at Existing Location 0.32 Miles East of Center Lamont Rd
81-14	_081E	47.63 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				
81-15	_081E	47.63 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
81-16R	_081E	48.39 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 81-16R
81-17R	_081E	48.39 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
81-18	_081E	48.48 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				
81-19	_081E	48.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				New Sign and Post 350' East of Existing 450 Feet West of Dobbs Rd
81-20R	_081E	51.27 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-21R	_081E	51.27 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									
81-22	_081E	51.34 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 250' East of Existing 650 Feet West of E Lamont Rd
81-23	_081E	51.34 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
81-24R	_081E	52.22 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-25R	_081E	52.22 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-24R
81-26R	_081E	53.08 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-27R	_081E	53.08 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									
81-28R	_081E	53.47 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as 81-28R
81-29R	_081E	53.47 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									
81-30	_081E	53.53 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				
81-31	_081E	53.53 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				New Sign and Post 300' East of Existing 0.48 Miles West of Apple Branch Rd (East Junction)
81-32R	_081E	53.87 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-33R	_081E	54.23 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
81-34R	_081E	54.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-33R
81-35	_081E	53.53 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				
81-36R	_081E	55.68 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	

3

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

Lafayette County STH 81 From STH 23 (Darlington) to Green County Continued STH 81 EB mi at STH 23 is 40.52. STH 81 WB mi at Lafayette County is 35.85.										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
Sign	Route	Marker	Position	Code	Description	Width	Height	Order Lines		Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
Number										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
										14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Sign	
										Each	Each	Each	SF	Each	Each	Supports	
81-37R	_081W	36.66 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
81-38R	_081W	38.23 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1	
81-39R	_081W	38.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-38R
81-40	_081W	38.48 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 0.46 Miles East of Apple Branch Rd (East Junction)
81-41R	_081W	38.71 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-42R	_081W	38.71 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-41R
81-43R	_081W	39.17 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-44R	_081W	39.17 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-43R
81-45R	_081W	39.53 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-46R	_081W	39.53 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-45R
81-47	_081W	39.84 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				New Sign and Post 0.90 Miles West of Apple Branch Rd (East Junction)
81-48R	_081W	40.39 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-49R	_081W	40.39 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-48R
81-50R	_081W	41.23 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-51R	_081W	41.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 81-50R
81-52	_081W	41.30 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 300' West of Existing 0.21 Miles West of Apple Grove Rd
81-53	_081W	41.30 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
81-54R	_081W	44.17 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-55R	_081W	44.17 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 81-54R
81-56	_081W	44.25 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 350' West of Existing 650 Feet East of Dobbs Rd
81-57	_081W	44.25 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
81-58R	_081W	44.91 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-59R	_081W	44.91 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									Attached to the Same Post as 81-58R
81-60	_081W	45.01 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 400' West of Existing 0.21 Miles West of Dobbs Rd
81-61	_081W	45.01 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
81-62R	_081W	46.04 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
81-63R	_081W	46.04 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									Attached to the Same Post as 81-62R
81-64R	_081W	46.97 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-65R	_081W	47.32 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
81-66R	_081W	48.05 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
81-67R	_081W	48.75 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-68R	_081W	49.19 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-69R	_081W	50.22 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
81-70R	_081W	51.26 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
81-71R	_081W	51.26 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									Attached to the Same Post as 81-70R
Segment Subtotals										4	7	0	84.50	0	34	34	

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Iowa County STH 191 From STH 23 (Dodgeville) to STH 39 (Hollandale) STH 191 EB mi at STH 23 is 0.00. STH 191 WB mi at STH 39 is 0.00.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									Notes
191-1R	_191E	3.02 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1		
191-2	_191E	3.06 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25					New Sign and Post 150' East of Existing 0.93 Miles East of CTH Z
191-3R	_191E	3.39 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1		
191-4R	_191E	3.39 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									On the Same Post as 191-3R
191-5R	_191E	3.50 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in							1	1		
191-6R	_191E	3.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									On the Same Post as 191-5R
191-7	_191E	3.67 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in			1		6.25					New Sign and Post 0.75 Miles West of CTH Y (West Junction)
191-8	_191E	3.67 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					
191-9R	_191E	4.01 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1		
191-10R	_191E	4.01 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 191-9R
191-11	_191E	4.01 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 150' East of Existing 0.35 Miles West of CTH Y (West Junction)
191-12	_191E	4.01 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					
191-13	_191E	4.01 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					New Sign and Post 0.26 Miles West of CTH Y (West Junction)
191-14R	_191E	5.51 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1		
191-15R	_191E	5.51 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 191-14R
191-16	_191E	5.55 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 150' East of Existing 0.21 Miles West of CTH BB
191-17	_191E	5.55 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25					
191-18	_191E	5.65 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00					New Sign and Post 0.12 Miles West of CTH BB
191-19R	_191E	6.36 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1		
191-20R	_191E	6.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									On the Same Post as 191-19R
191-21	_191E	6.47 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 400' East of Existing 0.54 Miles East of Spring Rd
191-22	_191E	6.47 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					
191-23R	_191E	6.64 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1		
191-24R	_191E	6.64 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									On the Same Post as 191-23R
191-25	_191E	6.73 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 350' East of Existing 0.80 Miles East of Spring Rd
191-26	_191E	6.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					
191-27R	_191E	7.10 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1		
191-28R	_191E	7.10 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									On the Same Post as 191-27R
191-29	_191E	7.19 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1		6.25					New Sign and Post 400' East of Existing 0.40 Miles West of CTH W
191-30	_191E	7.19 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25					
191-31R	_191E	7.46 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1		
191-32R	_191E	7.46 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 191-31R
191-33R	_191E	7.84 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1		
191-34R	_191E	7.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 191-33R
191-35	_191E	7.94 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25					New Sign and Post 400' East of Existing 0.35 Miles East of CTH W
191-36	_191E	7.94 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25					

3

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

3

Iowa County										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 191 From STH 23 (Dodgeville) to STH 39 (Hollandale) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 191 EB mi at STH 23 is 0.00. STH 191 WB mi at STH 39 is 0.00.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign								Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Signs	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines		Each	Each	Each	SF	Each	Each	Each	Notes
191-37R	_191E	8.11 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
191-38R	_191E	8.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 191-37R
191-39	_191E	8.15 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in					1	6.25				New Sign and Post 150' East of Existing
191-40	_191E	8.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				0.13 Miles West of Clay Hill Rd
191-41	_191E	8.22 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 200' West of Clay Hollow Rd
191-42	_191E	8.24 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 100' West of Clay Hollow Rd
191-43	_191E	8.26 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post at Northw est Corner of Clay Hollow Rd
191-44	_191E	8.28 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post at Northeast Corner of Clay Hollow Rd
191-45R	_191E	8.53 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
191-46R	_191E	8.53 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35									On the Same Post as 191-45R
191-47	_191E	8.59 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in					1	6.25				New Sign and Post 250' East of Existing
191-48	_191E	8.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				0.25 Miles East of Sunny Ridge Rd
191-49	_191E	8.64 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 0.29 Miles East of Sunny Ridge Rd
191-50	_191E	8.67 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 0.32 Miles East of Sunny Ridge Rd
191-51	_191E	8.69 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 0.34 Miles East of Sunny Ridge Rd
191-52	_191E	8.72 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 0.37 Miles East of Sunny Ridge Rd
191-53	_191E	8.74 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 0.39 Miles East of Sunny Ridge Rd
191-54R	_191E	9.28 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
191-55R	_191E	9.28 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									On the Same Post as 191-54R
191-56	_191E	9.50 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 0.27 Miles West of Squirrel Hill Rd
191-57	_191E	9.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
191-58R	_191E	10.43 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
191-59R	_191E	10.43 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									On the Same Post as 191-58R
191-60	_191E	10.52 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 450' East of Existing
191-61	_191E	10.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				0.52 Miles West of Black Oak Rd
191-62R	_191E	10.71 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in								1	1	
191-63R	_191E	10.71 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									On the Same Post as 191-62R
191-64	_191E	10.78 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 350' East of Existing
191-65	_191E	10.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.26 Miles West of Black Oak Rd
191-66R	_191E	11.02 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	
191-67R	_191E	11.68 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1	
191-68R	_191E	11.68 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50									On the Same Post as 191-67R
191-69	_191E	11.82 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 600' East of Existing
191-70R	_191E	12.03 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in								1	1	600 Feet West of Long Valley Rd
191-71R	_191E	12.03 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									On the Same Post as 191-70R

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Iowa County										638.3000										
STH 191 From STH 23 (Dodgeville) to STH 39 (Hollandale) Continued										634.0614		634.0616		634.0618		637.2230	638.2102	638.2602	Removing	
STH 191 EB mi at STH 23 is 0.00. STH 191 WB mi at STH 39 is 0.00.										Posts Wood		Posts Wood		Posts Wood		Signs	Moving	Removing	Small	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order	Lines	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Reflective F	Type II	Type II	Type II	Sign	Supports	Notes	
Number										14 Foot	16 Foot	18 Foot	SF	Each	Each	Each	Each	Each		
191-72	_191E	12.20 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in					1	9.00							New Sign and Post 750' East of Existing 0.19 Miles West of CTH K
191-73	_191E	12.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15					4.00							
191-74R	_191E	12.43 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in									1		1		On the Same Post as 191-74R
191-75R	_191E	12.43 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15												
191-76	_191E	12.50 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00							New Sign and Post 300' East of Existing 550' East of CTH K
191-77	_191E	12.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00							
191-78R	_191W	0.32 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in									1		1		On the Same Post as 191-78R
191-79R	_191W	0.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15												
191-80	_191W	0.32 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00							New Sign and Post at Same Location as Existing 0.18 Miles East of CTH K
191-81	_191W	0.32 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00							
191-82R	_191W	0.43 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in									1		1		On the Same Post as 191-82R
191-83R	_191W	0.43 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20												
191-84	_191W	0.49 mi	Right	W1-3-L	Left Reverse Turn	36.00 in	36.00 in					1	9.00							New Sign and Post 250' West of Existing 300' East of CTH K
191-85	_191W	0.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15					4.00							
191-86R	_191W	0.85 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in									1		1		On the Same Post as 191-86R
191-87R	_191W	0.85 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50												
191-88	_191W	0.89 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25							New Sign and Post 200' West of Existing 600' East of Long Valley Rd
191-89	_191W	0.89 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25							
191-90R	_191W	1.45 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in									1		1		On the Same Post as 191-91R
191-91R	_191W	1.78 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in									1		1		
191-92R	_191W	1.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45												
191-93	_191W	1.84 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in					1	6.25							New Sign and Post 150' West of Existing 350' East of Black Oak Rd
191-94	_191W	1.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25							
191-95R	_191W	2.07 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in									1		1		On the Same Post as 191-95R
191-96R	_191W	2.07 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50												
191-97R	_191W	3.08 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in									1		1		New Sign and Post 100' West of Squirrel Hill Rd
191-98R	_191W	3.08 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40												
191-99	_191W	3.20 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25							On the Same Post as 191-101R
191-100	_191W	3.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25							
191-101R	_191W	4.03 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in									1		1		New Sign and Post 350' West of Existing 0.47 Miles East of Sunny Ridge Rd
191-102R	_191W	4.03 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35												
191-103	_191W	4.13 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25							New Sign and Post 0.39 Miles East of Sunny Ridge Rd
191-104	_191W	4.13 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25							
191-105	_191W	4.22 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00							New Sign and Post 0.37 Miles East of Sunny Ridge Rd
191-106	_191W	4.25 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00							
191-107	_191W	4.27 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00							New Sign and Post 0.34 Miles East of Sunny Ridge Rd
191-108	_191W	4.30 mi	Left	W1-8	Chevron	18.00 in	24.00 in			1			3.00							

Iowa County															638.3000	
STH 191 From STH 23 (Dodgeville) to STH 39 (Hollandale) Continued															Removing	
STH 191 EB mi at STH 23 is 0.00. STH 191 WB mi at STH 39 is 0.00.															Small	
Sign								Order	634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	Supports	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines	4 x 6 - Inch 14 Foot Each	4 x 6 - Inch 16 Foot Each	4 x 6 - Inch 18 Foot Each	Signs Type II Reflective F SF	Moving Signs Type II Each	Removing Signs Type II Each	Each	Notes
191-109	_191W	4.32 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 0.29 Miles East of Sunny Ridge Rd
191-110R	_191W	4.49 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	On the Same Post as 191-110R
191-111R	_191W	4.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								
191-112	_191W	4.56 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 350' West of Existing 200' East of Sunny Ridge Rd
191-113	_191W	4.56 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25				
191-114	_191W	4.65 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post at Northeast Corner of Clay Hollow Rd
191-115	_191W	4.67 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post at Northw est Corner of Clay Hollow Rd
191-116	_191W	4.69 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 100' West of Clay Hollow Rd
191-117	_191W	4.71 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00				New Sign and Post 200' West of Clay Hollow Rd
191-118R	_191W	4.74 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	On the Same Post as 191-118R
191-119R	_191W	4.74 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								
191-120	_191W	4.86 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 450' West of Existing 0.20 Miles West of Clay Hill Rd
191-121	_191W	4.86 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
191-122R	_191W	4.99 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1	On the Same Post as 191-122R
191-123R	_191W	4.99 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								
191-124	_191W	5.42 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in					6.25		1		New Sign on Existing Post Remove Sign Only New Sign on Existing Post
191-125R	_191W	5.42 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45						1		
191-126	_191W	5.42 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
191-127R	_191W	5.73 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	1	On the Same Post as 191-127R
191-128R	_191W	5.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								
191-129	_191W	5.83 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 350' West of Existing 0.47 Miles West of CTH W
191-130	_191W	5.83 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
191-131R	_191W	6.17 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1	On the Same Post as 191-131R
191-132R	_191W	6.17 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								
191-133	_191W	6.28 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25				New Sign and Post 450' West of Existing 0.74 Miles East of Spring Rd
191-134	_191W	6.28 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
191-135R	_191W	6.95 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1	On the Same Post as 191-135R
191-136R	_191W	6.95 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								
191-137	_191W	7.08 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 450' West of Existing 0.74 Miles East of Spring Rd
191-138	_191W	7.08 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25				
191-139M	_191W	7.21 mi	Right	J4-1	Reassurance Assembly	24.00 in	36.00 in	WEST 191						1		Move Sign and Post 100' East to 50' West of CTH BB
191-140	_191W	7.23 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00				New Sign and Post 150' West of CTH BB
191-141R	_191W	7.26 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in							1	1	
191-142R	_191W	8.54 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	On the Same Post as 191-142R
191-143R	_191W	8.54 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35								

Iowa County																
STH 191 From STH 23 (Dodgeville) to STH 39 (Hollandale) Continued																
STH 191 EB mi at STH 23 is 0.00. STH 191 WB mi at STH 39 is 0.00.																
Sign										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000
Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines		Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing
										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign
										14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports
										Each	Each	Each	SF	Each	Each	Each
																Notes
191-144	_191W	8.63 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25			New Sign and Post 400' West of Existing
191-145	_191W	8.63 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25			500' West of CTH Y (West Junction)
191-146	_191W	8.72 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00			New Sign and Post 0.19 Miles West of CTH Y (West Junction)
191-147R	_191W	8.81 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1
191-148R	_191W	8.81 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								On the Same Post as 191-147R
191-149	_191W	8.88 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in				1		6.25			New Sign and Post 150' East of Existing
191-150	_191W	8.88 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25			0.35 Miles West of CTH Y (West Junction)
191-151R	_191W	8.91 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in								1	1
191-152R	_191W	8.91 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								On the Same Post as 191-151R
191-153R	_191W	9.65 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1
191-154R	_191W	11.84 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1
191-155R	_191W	11.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20								On the Same Post as 191-154R
191-156	_191W	11.97 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00			New Sign and Post 450' West of Existing
191-157	_191W	11.97 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00			500' East of Section Line Rd
191-158	_191W	12.05 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1	Replace Sign on Existing Post
Segment Subtotals										23	19	10	375.50	1	42	39

Dane, Green, Iowa and Lafayette Counties																
STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb)																
STH 78 NB mi at STH 81 is 19.66. STH 78 SB mi at USH 18 and 151 is 40.84.																
Sign										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000
Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines		Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing
										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Sign
										14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Supports
										Each	Each	Each	SF	Each	Each	Each
																Notes
78-1-1	_078N	21.20 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25			New Sign and Post
78-1-2R	_078N	21.94 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1
78-1-3R	_078N	22.17 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1
78-1-4R	_078N	22.43 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1
78-1-5R	_078N	24.27 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1
78-1-6	_078N	24.33 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25			New Sign and Post 250' North of Existing
78-1-7R	_078N	25.48 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in								1	1
78-1-8	_078N	25.59 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1				6.25			New Sign and Post 550' North of Existing
78-1-9R	_078N	26.31 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in								1	1
78-1-10	_078N	27.19 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25			New Sign and Post
78-1-11R	_078N	28.33 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1
78-1-12R	_078N	28.33 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15								Attached to the Same Post as Sign 178-11R
78-1-13	_078N	28.34 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00			New Sign and Post 150' North of Existing
78-1-14	_078N	28.34 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	10					4.00			250 Feet South of CTH H

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Dane, Green, Iowa and Lafayette Counties STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb) Continued STH 78 NB mi at STH 81 is 19.66. STH 78 SB mi at USH 18 and 151 is 40.84.															
								634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
								Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
								4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
								14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
								Each	Each	Each	SF	Each	Each	Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines							
78-1-15	_078N	28.37 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00			New Sign and Post 100 Feet South of CTH H
78-1-16	_078N	28.39 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00			New Sign and Post At CTH H Northeast Corner
78-1-17	_078N	28.41 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1			3.00			New Sign and Post 100 Feet North of CTH H
78-1-18R	_078N	28.93 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in						1	1	Attached to the Same Post as Sign 178-18R
78-1-19R	_078N	28.93 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45							
78-1-20	_078N	29.00 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1	6.25				New Sign and Post 400' North of Existing 100 Feet South of Dugout Road
78-1-21	_078N	29.00 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50			2.25				
78-1-22	_078N	33.72 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1	6.25				New Sign and Post 550 Feet South of CTH H
78-1-23	_078N	33.72 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50			2.25				
78-1-24R	_078N	33.94 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in						1	1	Attached to the Same Post as Sign 178-24R
78-1-25R	_078N	33.94 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30							
78-1-26	_078N	33.97 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1	6.25				New Sign and Post 100' North of Existing 650 Feet North of CTH H
78-1-27	_078N	33.97 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35			2.25				
78-1-28R	_078N	34.09 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in						1	1	Attached to the Same Post as Sign 178-28R
78-1-29R	_078N	34.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50							
78-1-30R	_078N	34.78 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in						1	1	Attached to the Same Post as Sign 178-30R
78-1-31R	_078N	34.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45							
78-1-32	_078N	33.97 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1	6.25				New Sign and Post 100' North of Existing 0.50 Miles South of Sherven Rd
78-1-33	_078N	33.97 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50			2.25				
78-1-34R	_078N	35.10 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in						1	1	Attached to the Same Post as Sign 178-34R
78-1-35R	_078N	35.10 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45							
78-1-36	_078N	35.14 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1	6.25				New Sign and Post 150' North of Existing 0.35 Miles South of Sherven Rd
78-1-37	_078N	35.14 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40			2.25				
78-1-38	_078N	35.22 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in				8.00		1		Replace Sign on Existing Post
78-1-39R	_078N	35.38 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in						1	1	Attached to the Same Post as Sign 178-39R
78-1-40R	_078N	35.38 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50							
78-1-41	_078N	35.44 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in			1	6.25				New Sign and Post 250' North of Existing 250 Feet South of Sherven Rd
78-1-42	_078N	35.44 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50			2.25				
78-1-43R	_078N	35.72 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in						1	1	Attached to the Same Post as Sign 178-43R
78-1-44R	_078N	35.72 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45							
78-1-45R	_078N	36.52 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in						1	1	Attached to the Same Post as Sign 178-45R New Sign and Post at Existing Location 300 Feet South of CTH A (South Junction)
78-1-46R	_078N	36.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35							
78-1-47	_078N	36.52 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in			1	9.00				
78-1-48	_078N	36.52 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30			4.00				
78-1-49	_078N	36.68 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1		3.00				New Sign and Post 350 Feet North of CTH A (South Junction)
78-1-50	_078N	36.70 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1		3.00				New Sign and Post 430 Feet North of CTH A (South Junction)

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PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

Dane, Green, Iowa and Lafayette Counties										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	Notes
STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Number								Lines		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
										Each	Each	Each	SF	Each	Each	Each	
78-1-51	_078N	36.71 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 510 Feet North of CTH A (South Junction)
78-1-52	_078N	36.73 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 590 Feet North of CTH A (South Junction)
78-1-53	_078N	36.75 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 670 Feet North of CTH A (South Junction)
78-1-54	_078N	36.76 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 750 Feet North of CTH A (South Junction)
78-1-55	_078N	36.78 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 830 Feet North of CTH A (South Junction)
78-1-56R	_078N	37.05 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in								1	1	
78-1-57R	_078N	37.05 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20									Attached to the Same Post as Sign 178-56R
78-1-58R	_078N	37.84 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in								1	1	
78-1-59R	_078N	37.84 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									Attached to the Same Post as Sign 178-58R
78-1-60	_078N	37.99 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 300' North of Existing
78-1-61	_078N	37.99 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				600 Feet South of CTH Z
78-1-62	_078N	38.22 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
78-1-63R	_078N	38.34 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
78-1-64R	_078N	38.34 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									Attached to the Same Post as Sign 178-63R
78-1-65	_078N	38.45 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in					1	6.25				New Sign and Post 400' North of Existing
78-1-66	_078N	38.45 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				0.34 Miles North of CTH Z
78-1-67R	_078N	38.78 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-1-68R	_078N	38.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									Attached to the Same Post as Sign 178-67R
78-1-69	_078N	38.90 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in					1	6.25				New Sign and Post 250' North of Existing
78-1-70	_078N	38.90 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				550 Feet North of Boley Rd
78-1-71R	_078N	39.60 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
78-1-72R	_078N	39.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40			1		6.25				Attached to the Same Post as Sign 178-71R
78-1-73	_078N	39.60 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in						2.25				New Sign and Post at Existing Location
78-1-74	_078N	39.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40									700 Feet South of CTH H
78-1-75	_078N	39.71 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in			1			8.00				New Sign and Post 100 Feet South of CTH H
78-1-76R	_078N	39.89 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
78-1-77R	_078N	39.89 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as Sign 178-76R
78-1-78	_078N	39.94 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 200' North of Existing
78-1-79	_078N	39.94 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				650 Feet South of Spring Valley Dr
78-1-80	_078N	40.02 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 250 Feet South of Spring Valley Dr
78-1-81	_078N	40.04 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 150 Feet South of Spring Valley Dr
78-1-82R	_078N	40.06 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in								1	1	
78-1-83	_078N	40.06 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post At Spring Valley Dr Southeast Corner
78-1-84	_078N	40.08 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post At Spring Valley Dr Northeast Corner
78-1-85	_078N	40.10 mi	Right	W1-8	Chevron	18.00 in	24.00 in			1			3.00				New Sign and Post 100 Feet North of Spring Valley Dr

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Dane, Green, Iowa and Lafayette Counties										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 78 NB mi at STH 81 is 19.66. STH 78 SB mi at USH 18 and 151 is 40.84.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small	
Sign	Route	Marker	Position	Code	Description	Width	Height	Order		14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Sign	Notes
Number								Lines		Each	Each	Each	SF	Each	Each	Supports	
78-1-86R	_078N	40.11 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
78-1-87R	_078N	40.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									Attached to the Same Post as Sign 178-86R
78-1-88	_078N	40.23 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 0.16 Miles North of Spring Valley Dr
78-1-89R	_078N	40.60 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-1-90R	_078N	40.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									Attached to the Same Post as Sign 178-89R
78-1-91	_078N	40.68 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 400' North of Existing 200 Feet North of Post Rd
78-1-92	_078N	40.68 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				
78-1-93	_078N	40.77 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 300 Feet South of Blue Valley Rd
78-1-94	_078N	40.79 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 200 Feet South of Blue Valley Rd
78-1-95	_078N	40.81 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 100 Feet South of Blue Valley Rd
78-1-96	_078N	40.83 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post At Blue Valley Rd
78-1-97	_078N	40.85 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 100 Feet North of Blue Valley Rd
78-1-98R	_078N	40.88 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
78-1-99R	_078N	40.88 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as Sign 178-98R
78-1-100	_078N	40.99 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 0.16 Miles North of Blue Valley Rd
78-1-101R	_078N	41.92 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-1-102R	_078N	41.92 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as Sign 178-101R
78-1-103	_078N	41.99 mi	Right	W1-5-R	Right Winding Road	30.00 in	30.00 in			1			6.25				New Sign and Post 400' North of Existing 100 Feet North of Thousand Rd
78-1-104	_078N	41.99 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				
78-1-105	_078N	42.08 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 50 Feet North of Blue Rock Rd
78-1-106R	_078N	42.28 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	
78-1-107R	_078N	42.28 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as Sign 178-106R
78-1-108	_078N	42.39 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 150 Feet South of Jelle Rd
78-1-109	_078N	42.08 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 300 Feet South of Sharp Rd
78-1-110R	_078N	42.74 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	
78-1-111R	_078N	42.74 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									Attached to the Same Post as Sign 178-110R
78-1-112	_078N	42.83 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 400' North of Existing 0.16 Miles North of Syvrud Rd
78-1-113	_078N	42.83 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
78-1-114R	_078S	42.78 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	
78-1-115R	_078S	42.78 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									Attached to the Same Post as Sign 178-114R
78-1-116	_078S	42.87 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in					1	6.25				New Sign and Post 350' South of Existing 0.15 Miles South of Barton Rd
78-1-117	_078S	42.87 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
78-1-118R	_078S	43.20 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	
78-1-119R	_078S	43.20 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									Attached to the Same Post as Sign 178-118R

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Dane, Green, Iowa and Lafayette Counties STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb) Continued STH 78 NB mi at STH 81 is 19.66. STH 78 SB mi at USH 18 and 151 is 40.84.										634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines									
78-1-120	_078S	43.31 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in						6.25				New Sign and Post 400' South of Existing
78-1-121	_078S	43.31 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40					2.25				400 Feet North of Sharp Rd
78-1-122	_078S	43.43 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
78-1-123	_078S	43.52 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 150 Feet North of Jelle Rd
78-1-124R	_078S	43.60 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-124R
78-1-125R	_078S	43.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									
78-1-126	_078S	43.70 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 0.15 Miles South of Jelle Rd
78-1-127	_078S	43.83 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in						8.00		1		Replace Sign on Existing Post
78-1-128R	_078S	44.72 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-128R
78-1-129R	_078S	44.72 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40									
78-1-130	_078S	44.80 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 300' South of Existing 450 Feet North of Thunder Ln
78-1-131	_078S	44.80 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				
78-1-132	_078S	44.91 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 100 Feet South of Thunder Ln
78-1-133R	_078S	45.00 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-133R
78-1-134R	_078S	45.00 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									
78-1-135	_078S	45.10 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 100 Feet North of Blue Valley Rd
78-1-136	_078S	45.12 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post At Blue Valley Rd
78-1-137	_078S	45.14 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 100 Feet South of Blue Valley Rd
78-1-138	_078S	45.16 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 200 Feet South of Blue Valley Rd
78-1-139	_078S	45.18 mi	Right	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 300 Feet South of Blue Valley Rd
78-1-140R	_078S	45.27 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-140R
78-1-141R	_078S	45.27 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									
78-1-142	_078S	45.65 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in			1			6.25				New Sign and Post 0.24 Miles North of Spring Valley Dr
78-1-143	_078S	45.65 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35					2.25				
78-1-144	_078S	45.71 mi	Right	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1				8.00				New Sign and Post 0.18 Miles North of Spring Valley Dr
78-1-145R	_078S	45.74 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-145R
78-1-146R	_078S	45.74 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35									
78-1-147	_078S	45.86 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 100 Feet North of Spring Valley Dr
78-1-148	_078S	45.88 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post At Spring Valley Dr Northeast Corner
78-1-149	_078S	45.90 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post At Spring Valley Dr Southeast Corner
78-1-150	_078S	45.92 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 150 Feet South of Spring Valley Dr
78-1-151	_078S	45.94 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 250 Feet South of Spring Valley Dr

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Dane, Green, Iowa and Lafayette Counties									634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb) Continued									Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 78 NB mi at STH 81 is 19.66. STH 78 SB mi at USH 18 and 151 is 40.84.									4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign								Order	14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number	Route	Marker	Position	Code	Description	Width	Height	Lines	Each	Each	Each	SF	Each	Each	Each	Notes
78-1-152R	_078S	46.09 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1	Attached to the Same Post as Sign 178-152R
78-1-153R	_078S	46.09 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	40								
78-1-154	_078S	46.15 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 200' South of Existing 400 Feet North of CTH H
78-1-155	_078S	46.15 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40				2.25				
78-1-156	_078S	46.15 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00				New Sign and Post At CTH H Southeast Corner Attached to the Same Post as Sign 178-157R
78-1-157R	_078S	46.79 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1	
78-1-158R	_078S	46.79 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50								New Sign and Post 450' South of Existing 250 Feet South of Hefty Rd
78-1-159	_078S	46.90 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				
78-1-160	_078S	46.90 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
78-1-161R	_078S	47.07 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1	Attached to the Same Post as Sign 178-161R New Sign and Post 350' South of Existing 100 Feet North of Boley Rd
78-1-162R	_078S	47.07 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45								
78-1-163	_078S	47.07 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in			1		6.25				Attached to the Same Post as Sign 178-165R New Sign and Post 450' South of Existing 0.18 Miles North of CTH Z
78-1-164	_078S	47.07 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25				
78-1-165R	_078S	47.48 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in							1	1	Replace Sign on Existing Post
78-1-166R	_078S	47.48 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30								
78-1-167	_078S	47.59 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in				1	9.00				Attached to the Same Post as Sign 178-170R New Sign and Post 600 Feet South of CTH A (North Junction)
78-1-168	_078S	47.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00				
78-1-169	_078S	47.71 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in					8.00		1		
78-1-170R	_078S	48.72 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1	Attached to the Same Post as Sign 178-174R New Sign and Post 300' South of Existing 0.26 Miles North of CTH A (South Junction)
78-1-171R	_078S	48.72 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20								
78-1-172	_078S	48.93 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 830 Feet North of CTH A (South Junction)
78-1-173	_078S	48.93 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
78-1-174R	_078S	49.03 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1	New Sign and Post 750 Feet North of CTH A (South Junction) New Sign and Post 670 Feet North of CTH A (South Junction) New Sign and Post 590 Feet North of CTH A (South Junction) New Sign and Post 510 Feet North of CTH A (South Junction)
78-1-175R	_078S	49.03 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	35								
78-1-176	_078S	49.11 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 430 Feet North of CTH A (South Junction) New Sign and Post 350 Feet North of CTH A (South Junction)
78-1-177	_078S	49.11 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	35				2.25				
78-1-178	_078S	49.19 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				Attached to the Same Post as Sign 178-185R
78-1-179	_078S	49.21 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				
78-1-180	_078S	49.22 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				
78-1-181	_078S	49.24 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				
78-1-182	_078S	49.26 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				
78-1-183	_078S	49.27 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				
78-1-184	_078S	49.29 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1			3.00				
78-1-185R	_078S	49.91 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1	
78-1-186R	_078S	49.91 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45								

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PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

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Dane, Green, Iowa and Lafayette Counties										634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000	
STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb) Continued										Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing	
STH 78 NB mi at STH 81 is 19.66. STH 78 SB mi at USH 18 and 151 is 40.84.										4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Signs	
Sign										14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Type II	
Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines		Each	Each	Each	SF	Each	Each	Each	Notes
78-1-187	_078S	50.04 mi	Right	W1-5-L	Left Winding Road	30.00 in	30.00 in					1	6.25				New Sign and Post 300' South of Existing 0.43 Miles South of Sherven Rd
78-1-188	_078S	50.04 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
78-1-189R	_078S	50.13 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-189R
78-1-190R	_078S	50.13 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									
78-1-191	_078S	50.51 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 300' North of Existing 150 Feet North of Sherven Rd
78-1-192	_078S	50.51 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
78-1-193R	_078S	50.55 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-193R
78-1-194R	_078S	50.55 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45									
78-1-195R	_078S	50.75 mi	Right	W1-4-L	Left Reverse Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-195R
78-1-196R	_078S	50.75 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									
78-1-197	_078S	50.81 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 200' South of Existing 0.34 Miles South of Sherven Rd
78-1-198	_078S	50.81 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45					2.25				
78-1-199R	_078S	51.50 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-199R
78-1-200R	_078S	51.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50									
78-1-201R	_078S	51.75 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-201R
78-1-202R	_078S	51.75 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30									
78-1-203	_078S	51.77 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in					1	9.00				New Sign and Post 100' South of Existing 0.35 Miles North of CTH H
78-1-204	_078S	51.77 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30					4.00				
78-1-205R	_078S	51.92 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in								1	1	Attached to the Same Post as Sign 178-205R
78-1-206R	_078S	51.92 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									
78-1-207M	_078S	51.95 mi	Right	J1-1	Junction or End Assembly	24.00 in	39.00 in	JCT H						1			Move Existing Sign and Post 100' North
78-1-208	_078S	51.98 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in					1	6.25				
78-1-209	_078S	51.98 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				New Sign and Post 300' South of Existing 0.14 Miles North of CTH H
78-1-210	_078S	55.58 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1				6.25				
78-1-211R	_078S	56.63 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in								1	1	Attached to the Same Post as Sign 178-211R
78-1-212R	_078S	56.63 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45									
78-1-213	_078S	56.72 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1		6.25				New Sign and Post 500' South of Existing 0.19 Miles South of Moscow Rd
78-1-214	_078S	56.72 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50					2.25				
78-1-215	_078S	57.50 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in						9.00		1		Replace Signs on Existing Post
78-1-216	_078S	57.50 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	15					4.00				
78-1-217	_078S	57.56 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post 100 Feet North of CTH H
78-1-218	_078S	57.58 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				
78-1-219	_078S	57.60 mi	Left	W1-8	Chevron	18.00 in	24.00 in		1				3.00				New Sign and Post At CTH H Southeast Corner
78-1-220	_078S	57.68 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in					1	9.00				
78-1-221	_078S	57.68 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20					4.00				New Sign and Post 500 Feet South of CTH H

3

PROJECT NO: 1000-20-80

HWY: VARIOUS

COUNTY: VARIOUS – SW REGION

MISCELLANEOUS QUANTITIES

SHEET NO:

E

Dane, Green, Iowa and Lafayette Counties														638.3000	
STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb) Continued														Removing Small Sign Supports Each	
STH 78 NB mi at STH 81 is 19.66. STH 78 SB mi at USH 18 and 151 is 40.84.															
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	Notes
78-1-222	_078S	59.09 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25			New Sign and Post 0.42 Miles South of Pike Peak Ln
78-1-223R	_078S	59.27 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	
78-1-224R	_078S	59.70 mi	Right	W1-4-L	Left Reverse Curve	30.00 in	30.00 in							1	
78-1-225	_078S	61.20 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25			New Sign and Post 0.26 Miles North of Saw mill Rd
78-1-226R	_078S	61.30 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	
78-1-227R	_078S	63.04 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	
78-1-228R	_078S	63.40 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	
78-1-229R	_078S	64.97 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	
Segment Subtotals									56	22	13	612.25	1	61	55

Lafayette County														638.3000	
STH 78 From Illinois to STH 11 (Gratiot)														Removing Small Sign Supports Each	
STH 78 NB mi at Illinois is 0.00. STH 78 SB mi at CTH D is 40.84.															
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	Notes
78-2-1R	_078N	0.70 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	Attached to the Same Post as Sign 278-1R
78-2-2R	_078N	0.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50							
78-2-3	_078N	0.73 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25			New Sign and Post 100' North of Existing 450 Feet South of CTH W
78-2-4	_078N	0.73 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			
78-2-5R	_078N	2.89 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	New Sign and Post 300' North of Existing 0.31 Miles North of White School Rd
78-2-6	_078N	2.93 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25			
78-2-7R	_078N	3.73 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	
78-2-8	_078N	3.84 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25			New Sign and Post 500' North of Existing 0.62 Miles South of White Dunbarton Rd
78-2-9R	_078N	3.96 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	
78-2-10R	_078N	4.74 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50						1	Remove Speed Advisory From Below Existing Left Curve Sigr
78-2-11R	_078N	5.18 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	
78-2-12R	_078S	80.27 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	
78-2-13R	_078S	80.68 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	
78-2-14	_078S	80.78 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25			New Sign and Post 500' South of Existing 250 Feet South of Gratiot Rd Remove Speed Advisory From Below Existing Left Curve Sigr
78-2-15R	_078S	81.38 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45						1	
78-2-16	_078S	81.38 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			New Sign on Existing Post Below Existing Left Curve Sign
78-2-17R	_078S	81.61 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	
78-2-18	_078S	81.75 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25			New Sign and Post 650' South of Existing 0.25 Miles South of Dunbarton Rd
78-2-19R	_078S	81.89 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	

Lafayette County															638.3000	
STH 78 From Illinois to STH 11 (Gratiot) Continued															Removing Small Sign Supports	
STH 78 NB mi at Illinois is 0.00. STH 78 SB mi at CTH D is 40.84.																
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
78-2-20R	_078S	82.71 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
78-2-21	_078S	82.80 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 400' South of Existing 0.54 Miles North of White School Rd
78-2-22R	_078S	84.97 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
78-2-23R	_078S	84.97 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								Attached to the Same Post as Sign 278-22R
78-2-24	_078S	84.97 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25				New Sign and Post at Existing Location
78-2-25	_078S	84.97 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45				2.25				750 Feet North of CTH W
Segment Subtotals									4	3	0	50.50	0	13	11	

Dane and Green Counties															638.3000	
STH 92 From CTH G (Mt Vernon) to USH 14 (East of Brooklyn)															Removing Small Sign Supports	
STH 92 EB mi at CTH G is 6.41. STH 92 WB mi at USH 14 is 0.00.																
Sign Number	Route	Marker	Position	Code	Description	Width	Height	Order Lines	634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each	634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each	634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each	637.2230 Signs Type II Reflective F SF	638.2102 Moving Signs Type II Each	638.2602 Removing Signs Type II Each	638.3000 Removing Small Sign Supports Each	Notes
92-1R	_092E	7.60 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1	
92-2R	_092E	8.13 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
92-3R	_092E	8.13 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50								Attached to the Same Post as Sign 92-2R
92-4	_092E	8.23 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25				New Sign and Post 450' East of Existing 0.13 Miles West of Spring Rose Rd
92-5	_092E	8.23 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25				
92-6R	_092E	11.90 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
92-7	_092E	12.68 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in					6.25		1		Replace Sign on Existing Post
92-8R	_092E	14.01 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in							1		
92-9R	_092E	14.01 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25								Attached to the Same Post as Sign 92-8R
92-10M	_092E	14.01 mi	Right	J4-1	Reassurance Assembly	24.00 in	36.00 in	EAST 92								Install Moved Sign on Existing Post
92-11	_092E	14.07 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00				New Sign and Post 150 Feet East of Vorndran Dr, Belleville
92-12	_092E	14.07 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20				4.00				Move Sign 250' West to Existing Post
92-13M	_092E	14.07 mi	Right	J4-1	Reassurance Assembly	24.00 in	36.00 in	EAST 92					1		2	
92-14M	_092E	14.07 mi	Right	D2-1	Destination Distance	66.00 in	15.00 in	Brooklyn 12					1			Move Sign 250' East to New Posts
92-15M	_092E	14.12 mi	Right	D2-1	Destination Distance	66.00 in	15.00 in	Brooklyn 12	2							New Location 100 Feet West of Vorndran Dr, Belleville
92-16R	_092E	14.51 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1	
92-17R	_092E	15.93 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1	
92-18R	_092E	15.93 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20								Attached to the Same Post as Sign 92-17R
92-19	_092E	16.00 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00				New Sign and Post 350' East of Existing 600 Feet West of CTH W
92-20	_092E	16.00 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25				4.00				
92-21	_092E	16.56 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 0.22 Miles West of Dayton St
92-22R	_092E	16.59 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1	
92-23	_092E	16.71 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in		1			6.25				New Sign and Post 300 Feet West of Dayton St
92-24R	_092E	18.92 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1	
92-25R	_092E	18.92 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40								Attached to the Same Post as Sign 92-24R

3

Dane and Green Counties															
STH 92 From CTH G (Mt Vernon) to USH 14 (East of Brooklyn) Continued															
STH 92 EB mi at CTH G is 6.41. STH 92 WB mi at USH 14 is 0.00.															
Sign								Order	634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000
Number	Route	Marker	Position	Code	Description	Width	Height	Lines	Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing
									4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small
									14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Sign
									Each	Each	Each	SF	Each	Each	Supports
															Notes
92-26	_092E	19.08 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25			New Sign and Post
92-27	_092E	19.08 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			350 Feet West of Sandy Hook Rd
92-28R	_092E	19.21 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	
92-29	_092E	19.30 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in		1			6.25			New Sign and Post 400' East of Existing
92-30R	_092E	21.59 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1
92-31R	_092E	21.59 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	50							Attached to the Same Post as Sign 92-30R
92-32R	_092E	23.39 mi	Right	W1-2-L	Left Curve	36.00 in	36.00 in							1	1
92-33R	_092E	23.39 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45							Attached to the Same Post as Sign 92-32R
92-34	_092E	23.49 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in				1	6.25			New Sign and Post 500' East of Existing
92-35	_092E	23.49 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			0.36 Miles East of Glenw ay Rd
92-36R	_092E	24.66 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in							1	1
92-37	_092E	24.82 mi	Left	W1-6	Night Arrow (Single)	48.00 in	24.00 in		1			8.00			New Sign and Post
92-38R	_092E	26.63 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
92-39R	_092W	0.05 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
92-40R	_092W	1.97 mi	Right	W1-3-R	Right Reverse Turn	36.00 in	36.00 in							1	1
92-41R	_092W	3.29 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1
92-42R	_092W	3.29 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	45							Attached to the Same Post as Sign 92-41R
92-43	_092W	3.41 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in			1		6.25			New Sign and Post 550' West of Existing
92-44	_092W	3.41 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			0.50 Miles East of Glenw ay Rd
92-45R	_092W	4.80 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1
92-46R	_092W	4.80 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50							Attached to the Same Post as Sign 92-45R
92-47R	_092W	7.34 mi	Right	W1-2-R	Right Curve	36.00 in	36.00 in							1	1
92-48R	_092W	7.67 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
92-49R	_092W	7.67 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	40							Attached to the Same Post as Sign 92-48R
92-50	_092W	7.80 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25			New Sign and Post 550' West of Existing
92-51	_092W	7.80 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			450 Feet East of Sandy Hook Rd
92-52R	_092W	9.93 mi	Right	W1-4-R	Right Reverse Curve	30.00 in	30.00 in							1	1
92-53R	_092W	10.64 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in							1	1
92-54R	_092W	10.64 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20							Attached to the Same Post as Sign 92-53R
92-55	_092W	10.80 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00			New Sign and Post 650' West of Existing
92-56	_092W	10.80 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	20				4.00			500 Feet East of CTH W
92-57R	_092W	11.96 mi	Right	W1-4-R	Right Reverse Curve	36.00 in	36.00 in							1	1
92-58R	_092W	12.60 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in							1	1
92-59R	_092W	12.60 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25							Attached to the Same Post as Sign 92-58R
PROJECT NO: 1000-20-80			HWY: VARIOUS			COUNTY: VARIOUS – SW REGION			MISCELLANEOUS QUANTITIES			SHEET NO:			E





Dane and Green Counties															
STH 92 From CTH G (Mt Vernon) to USH 14 (East of Brooklyn) Continued															
STH 92 EB mi at CTH G is 6.41. STH 92 WB mi at USH 14 is 0.00.															
Sign									634.0614	634.0616	634.0618	637.2230	638.2102	638.2602	638.3000
Number	Route	Marker	Position	Code	Description	Width	Height	Order	Posts Wood	Posts Wood	Posts Wood	Signs	Moving	Removing	Removing
								Lines	4 x 6 - Inch	4 x 6 - Inch	4 x 6 - Inch	Type II	Signs	Signs	Small
									14 Foot	16 Foot	18 Foot	Reflective F	Type II	Type II	Sign
									Each	Each	Each	SF	Each	Each	Supports
															Notes
92-60	_092W	12.70 mi	Right	W1-1-L	Left Turn	36.00 in	36.00 in				1	9.00			New Sign and Post 400' West of Existing 0.16 Miles East of Remy Rd
92-61	_092W	12.70 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	25				4.00			
92-62R	_092W	14.00 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
92-63R	_092W	14.56 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
92-64R	_092W	18.37 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in							1	1
92-65R	_092W	18.37 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	45							Attached to the Same Post as Sign 92-64R
92-66	_092W	18.47 mi	Right	W1-2-L	Left Curve	30.00 in	30.00 in			1		6.25			
92-67	_092W	18.47 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	18.00 in	18.00 in	50				2.25			New Sign and Post 400' West of Existing 0.20 Miles West of Spring Rose Rd
92-68R	_092W	18.96 mi	Right	W1-2-R	Right Curve	30.00 in	30.00 in							1	1
92-69	_092W	20.36 mi	Right	W1-1-R	Right Turn	36.00 in	36.00 in				1	9.00		1	1
92-70	_092W	20.36 mi	Right	W13-1	Advisory Speed Plate (Yellow Back)	24.00 in	24.00 in	30				4.00			Replace Signs and Post at Existing Location 0.18 Miles East of Jackson St
Segment Subtotals									6	5	6	149.00	2	29	29

638.3000							
Removing Small Sign Supports							
638.2602 Removing Signs Type II Each							
638.2102 Moving Signs Type II Each							
637.2230 Signs Type II Reflective F SF							
634.0618 Posts Wood 4 x 6 - Inch 18 Foot Each							
634.0616 Posts Wood 4 x 6 - Inch 16 Foot Each							
634.0614 Posts Wood 4 x 6 - Inch 14 Foot Each							
Segment Summary							
11-1	STH 11 From STH 104 (Brodhead) to CTH WC (Beloit)	2	0	0	12.50	0	6
106	STH 106 From STH 73 (East of Albion) to STH 59 (Palmyra)	19	16	5	351.00	0	47
16-2	STH 16 at STH 19	0	0	0	0.00	0	1
89	STH 89 From USH 12 (Ft Atkinson) to CTH A South Junction (Lake Mills)	4	2	0	42.00	1	7
89-1	STH 89 From CTH G (North of Lake Mills) to STH 73 (Columbus)	3	12	0	138.00	2	19
16	STH 16 From STH 73 (Columbus) to CTH T	0	0	0	0.00	0	4
16-1	STH 16 From USH 51 (South of Portage) to Long Crossing Rd (East of Rio)	1	0	0	8.00	0	1
146	STH 146 From STH 16 (East of Fall River) to STH 33 (North of Cambria)	33	14	3	380.50	0	42
33-1	STH 33 From USH 51 and STH 16 (Portage) to STH 73 (North of Randolph)	1	0	0	14.25	0	20
44	STH 44 From STH 22 (Pardeeville) to Green Lake County	22	5	6	192.00	0	16
127	STH 127 From STH 16 (East of Wisconsin Dells) to IH 39 (Portage)	1	7	5	150.00	0	24
78	STH 78 From CTH DL (North of Merrimac) to IH 39 (South of Portage)	16	12	15	366.25	0	33
188	STH 188 From USH 12 (Sauk City) to STH 113 (Merrimac)	6	3	13	259.75	2	24
113	STH 113 From STH 78 (West of Merrimac) to STH 123 (Baraboo)	9	3	3	257.00	0	29
136	STH 136 From STH 33 (East of Reedsburg) to STH 154 (Rock Springs)	2	3	1	67.00	1	14
33	STH 33 From STH 80 (Union Center) to STH 23 (Reedsburg)	2	1	1	51.75	0	31
71	STH 71 From STH 16 (Sparta) to STH 80 (Elroy)	7	0	0	59.00	0	8
80-1	STH 80 From CTH D North Junction (Rockbridge) to STH 33 (Hillsboro)	0	2	2	63.00	0	10
130	STH 130 From Richland County (Bear Valley) to STH 154 (West of Loganville)	10	13	7	246.00	4	28
60	STH 60 From USH 14 (Spring Green) to CTH B (West of Sauk City)	6	8	2	136.75	0	27
80	STH 80 From USH 18 (Cobb) to STH 133 (East of Muscoda)	19	6	0	124.00	0	20
133	STH 133 From CTH T (Blue River) to STH 80 (Muscoda)	0	0	0	0.00	0	5
35	STH 35 From STH 81 (West of Lancaster) to USH 18 (Patch Grove)	2	0	0	25.00	1	11
11	STH 11 From STH 80 (North of Hazel Green) to CTH O (Shullsburg)	2	1	0	34.75	0	8
23	STH 23 From STH 11 (South of Darlington) to CTH ZZ (North of Dodgeville)	2	1	1	46.00	0	10
81	STH 81 From STH 23 (Darlington) to Green County	4	7	0	84.50	0	34
191	STH 191 From STH 23 (Dodgeville) to STH 39 (Hollandale)	23	19	10	375.50	1	42
78-1	STH 78 From STH 81 (Argyle) to USH 18 and 151 (Mt Horeb)	56	22	13	612.25	1	61
78-2	STH 78 From Illinois to STH 11 (Gratiot)	4	3	0	50.50	0	13
92	STH 92 From CTH G (Mt Vernon) to USH 14 (East of Brooklyn)	6	5	6	149.00	2	29
Grand Totals		262	165	93	4296.25	15	636

Standard Detail Drawing List

15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15D27-02	TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH
15D28-03	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

LEGEND

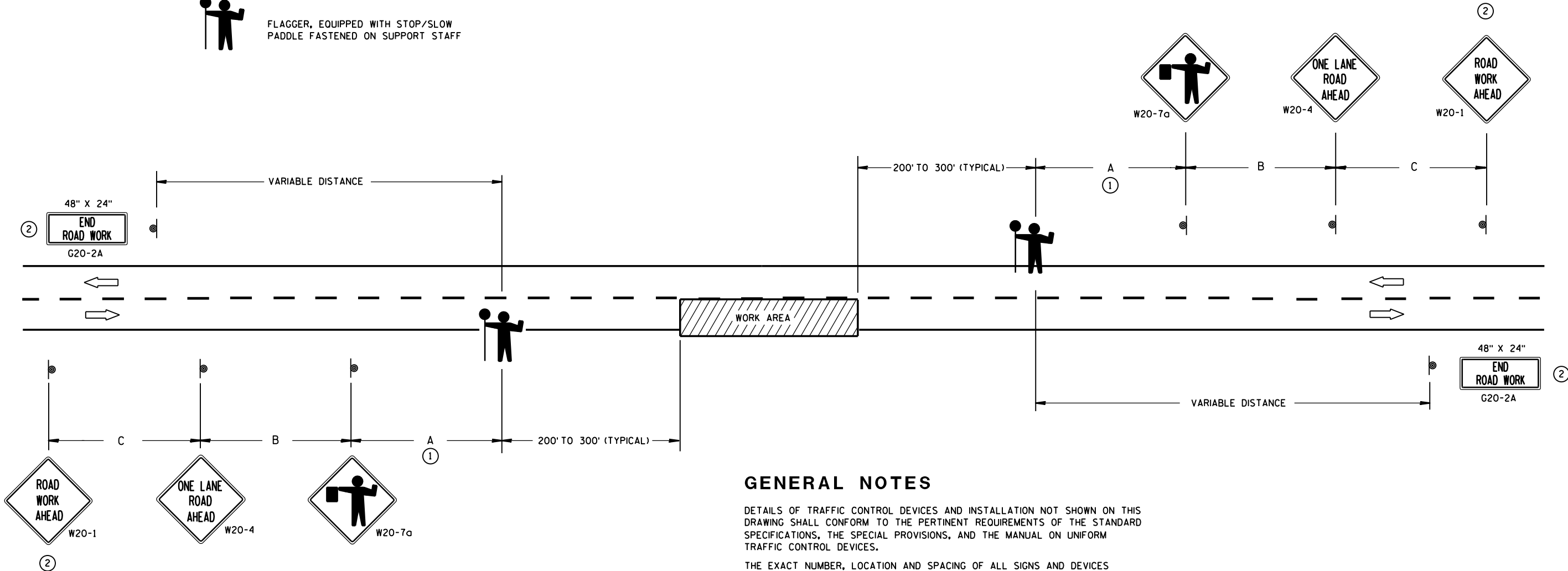
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ⚡➡ FLASHING ARROW BOARD
- ▨ WORK AREA

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

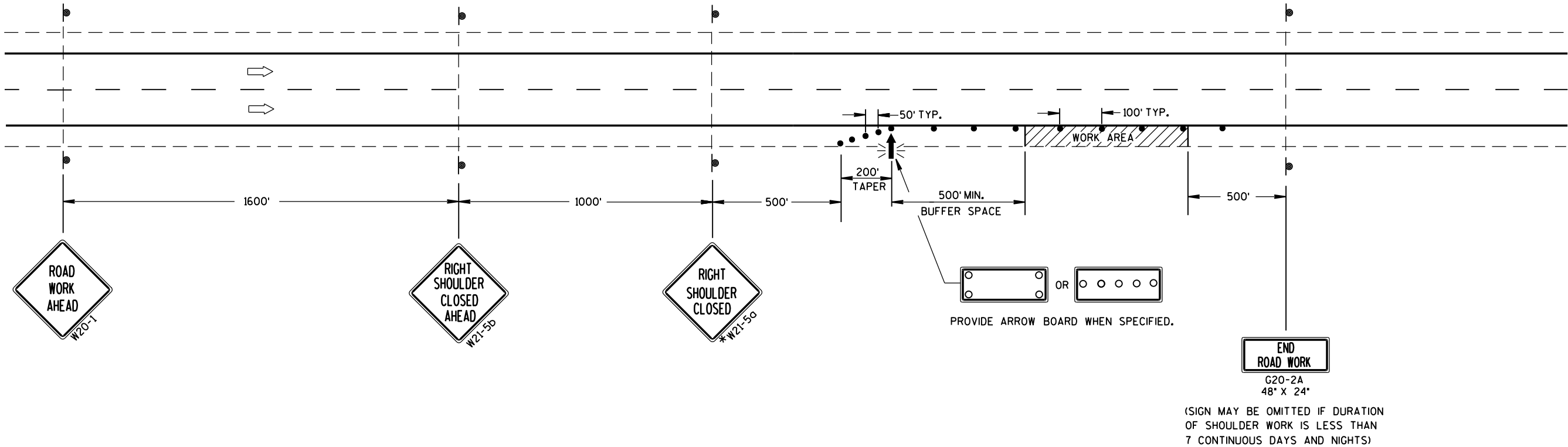
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-5a SIGN MAY BE OMITTED.



TRAFFIC CONTROL
SHOULDER CLOSURE ON DIVIDED
ROADWAY, SPEEDS GREATER
THAN 40 MPH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltz
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

TABLE A

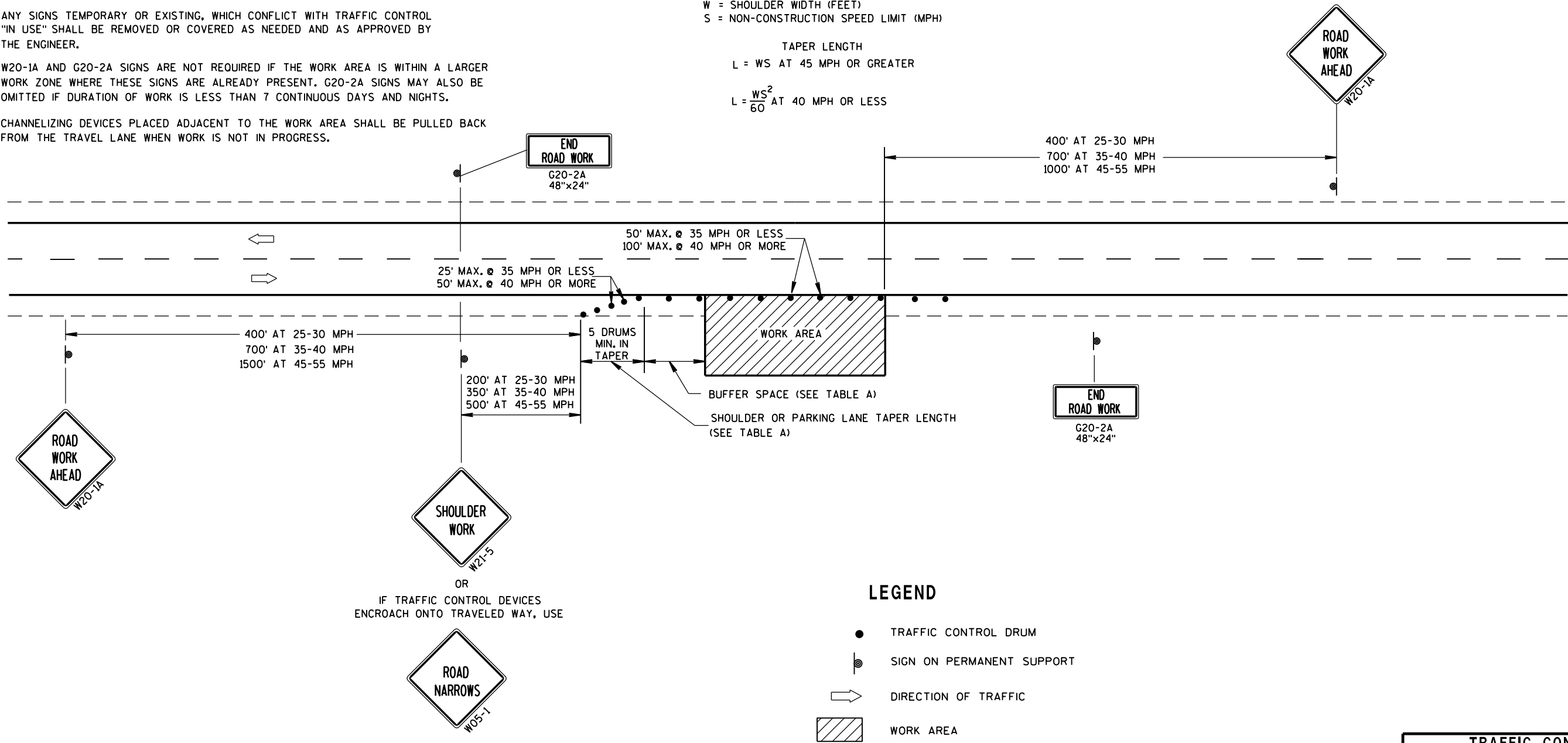
SHOULDER TAPER LENGTH (FEET)					BUFFER SPACE (FEET)
S \ W	4	6	8	10	
30	20	30	40	50	200
35	30	45	55	70	250
40	40	55	75	90	305
45	60	90	120	150	360
50	70	100	135	170	425
55	75	110	150	185	495

W = SHOULDER WIDTH (FEET)
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

TAPER LENGTH
L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

SHOULDER TAPER LENGTH = $\frac{1}{3}L$

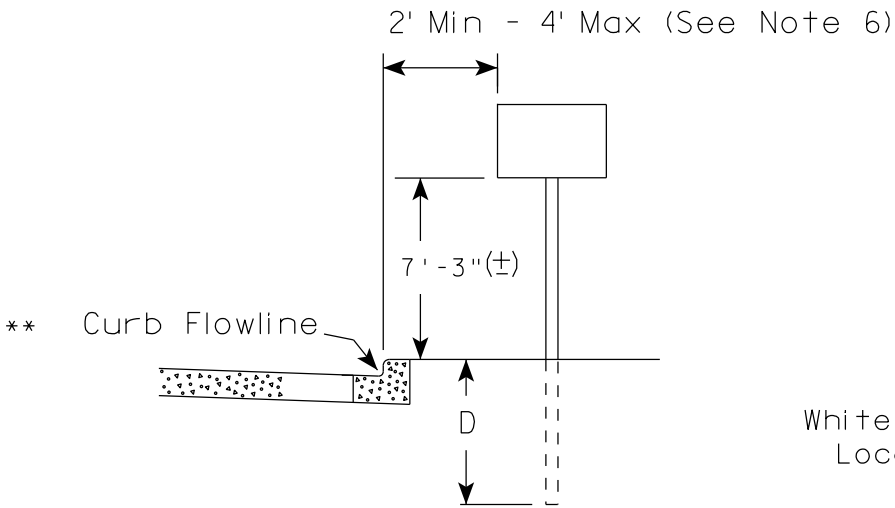


LEGEND

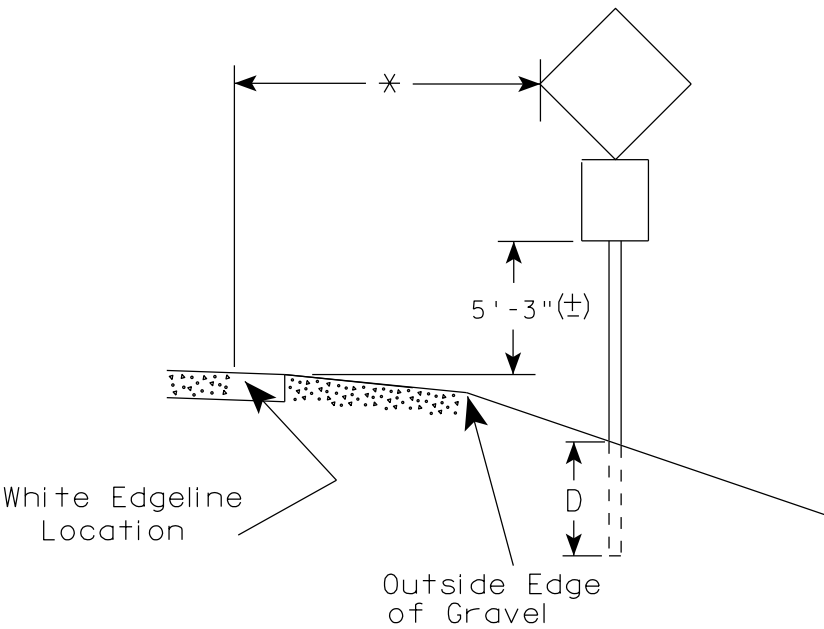
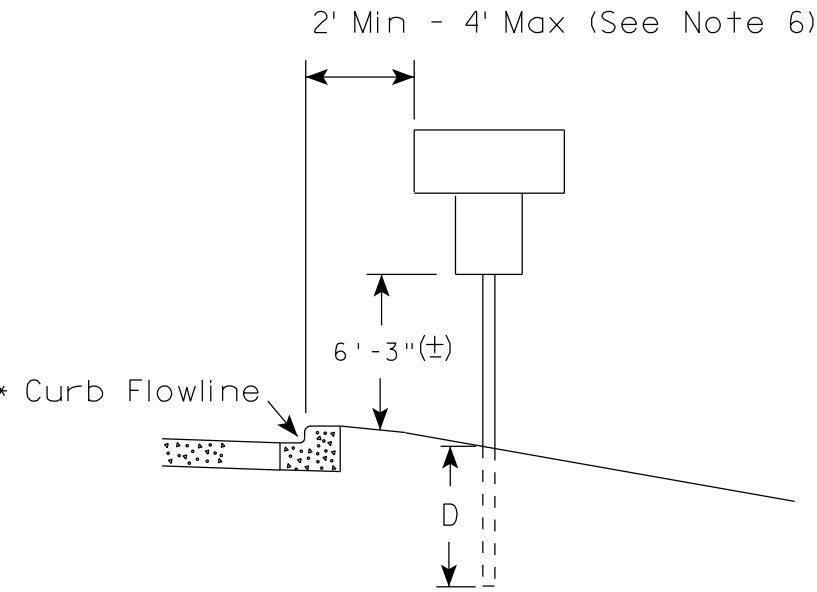
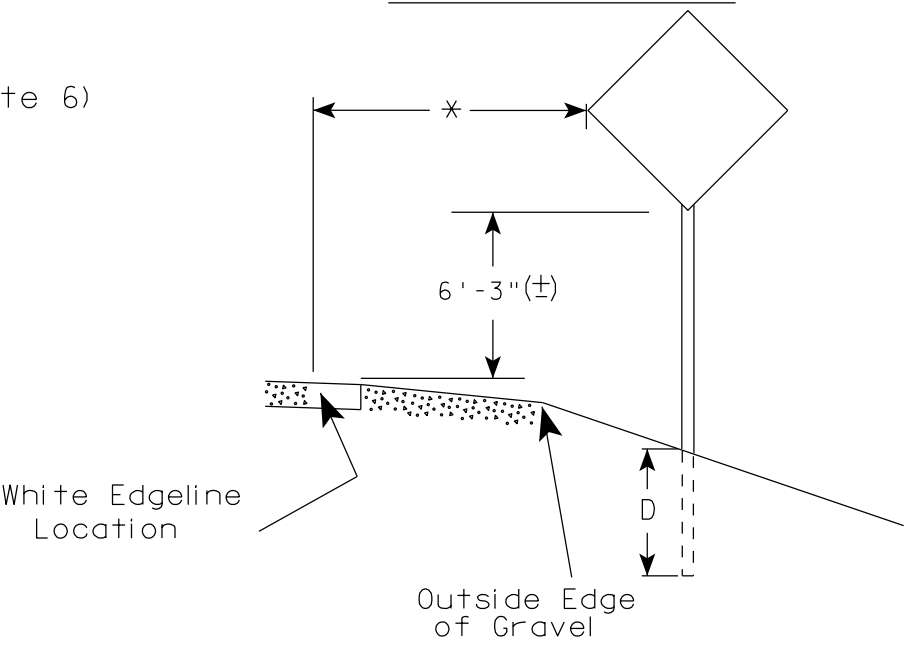
- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 14, 2015 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'-3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

×× The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

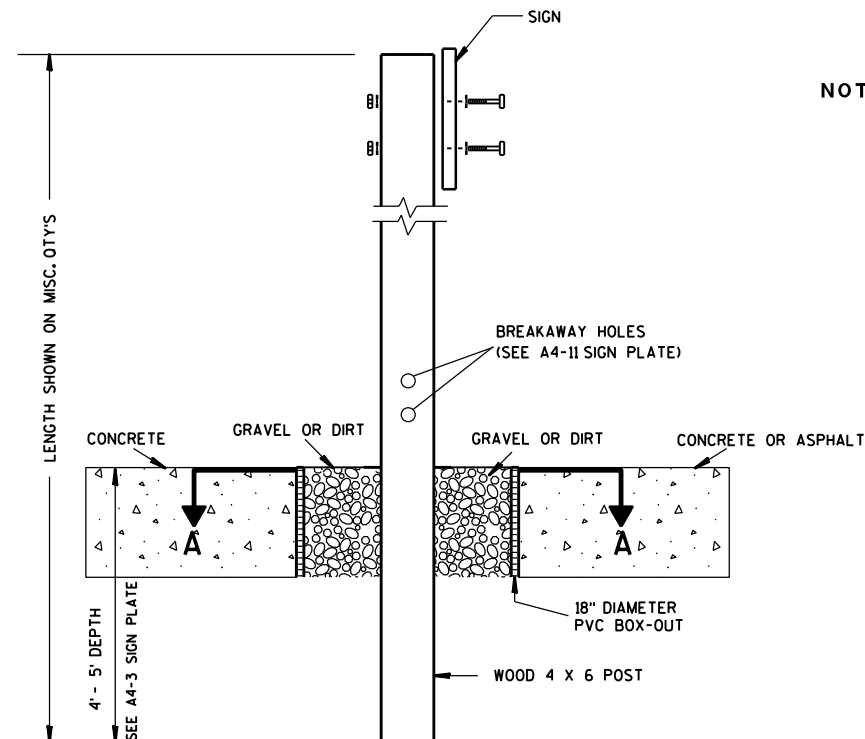
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

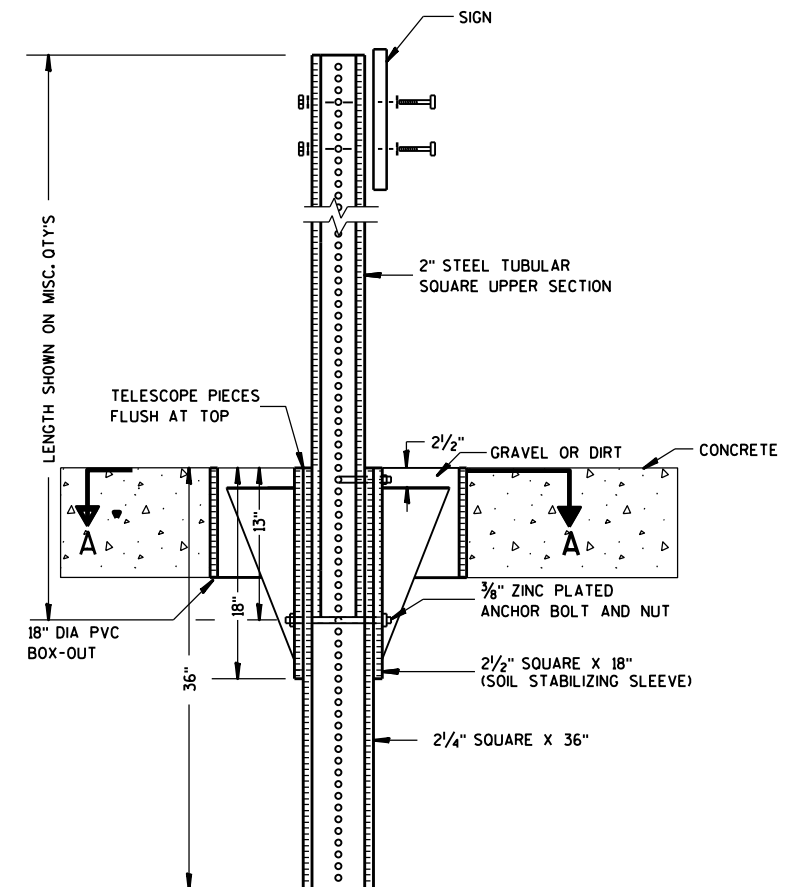
DATE 7/23/15 PLATE NO. A4-3.20



ELEVATION VIEW

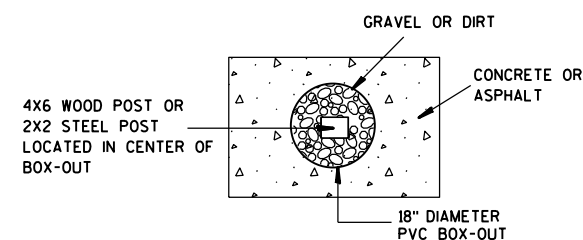
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

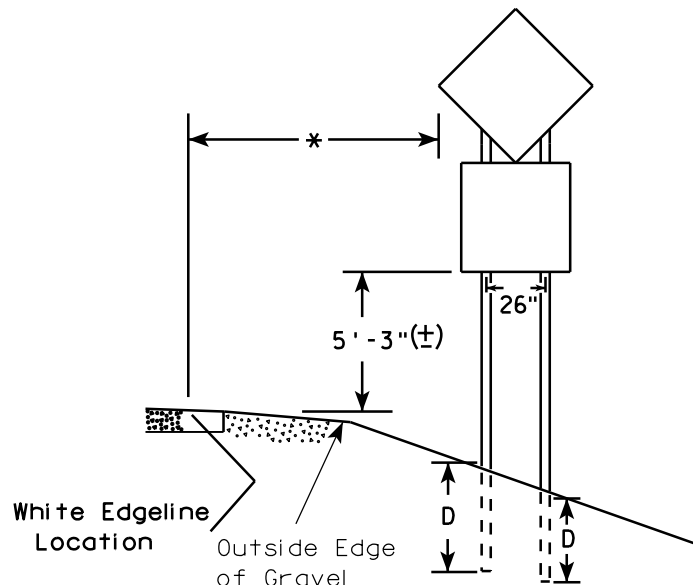
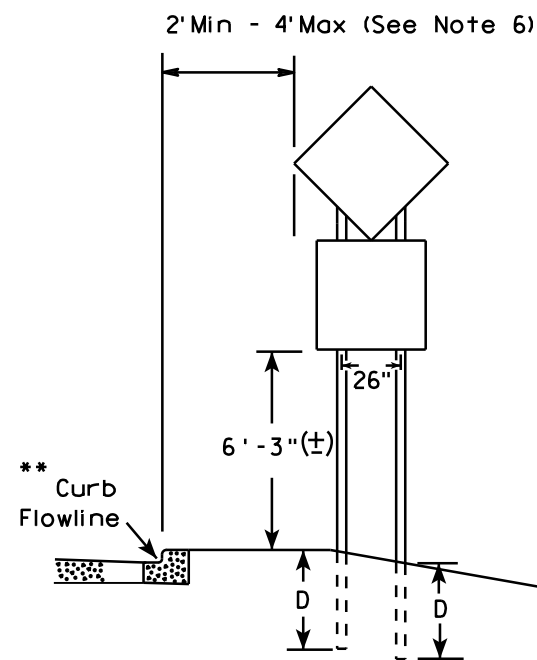
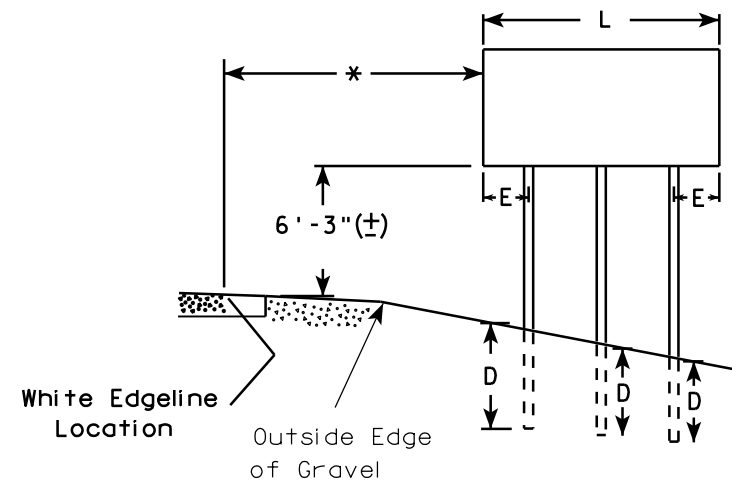
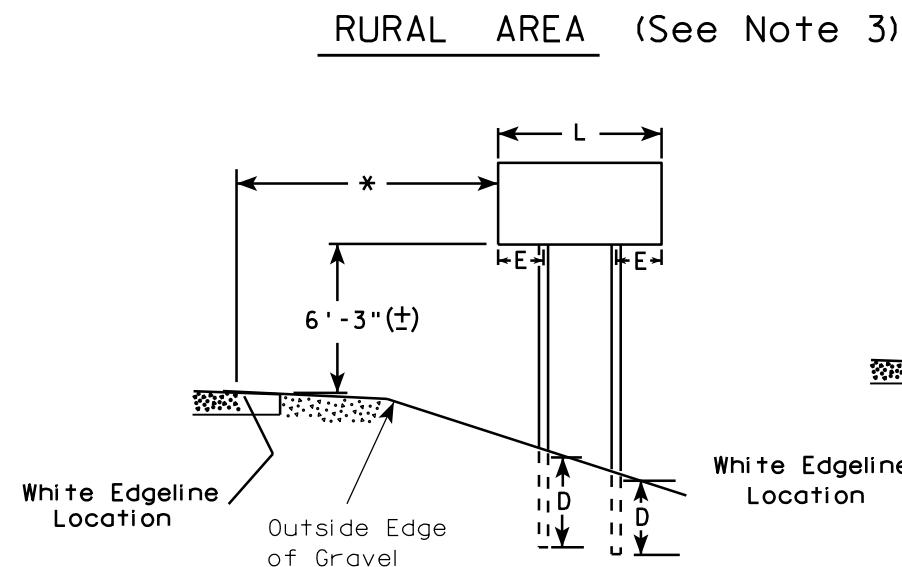
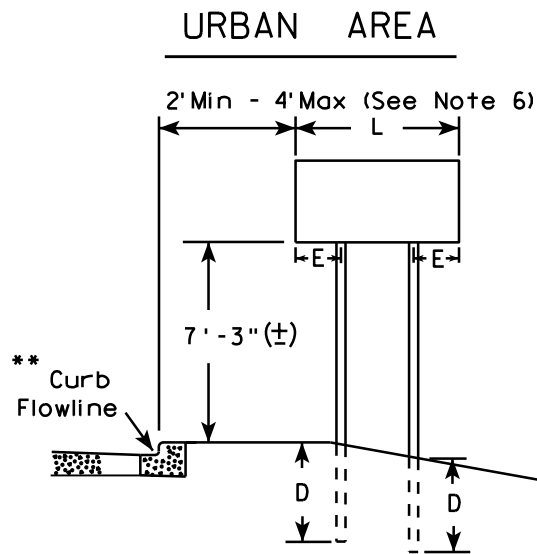
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



48" DIAMOND WARNING SIGN

- GENERAL NOTES**
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

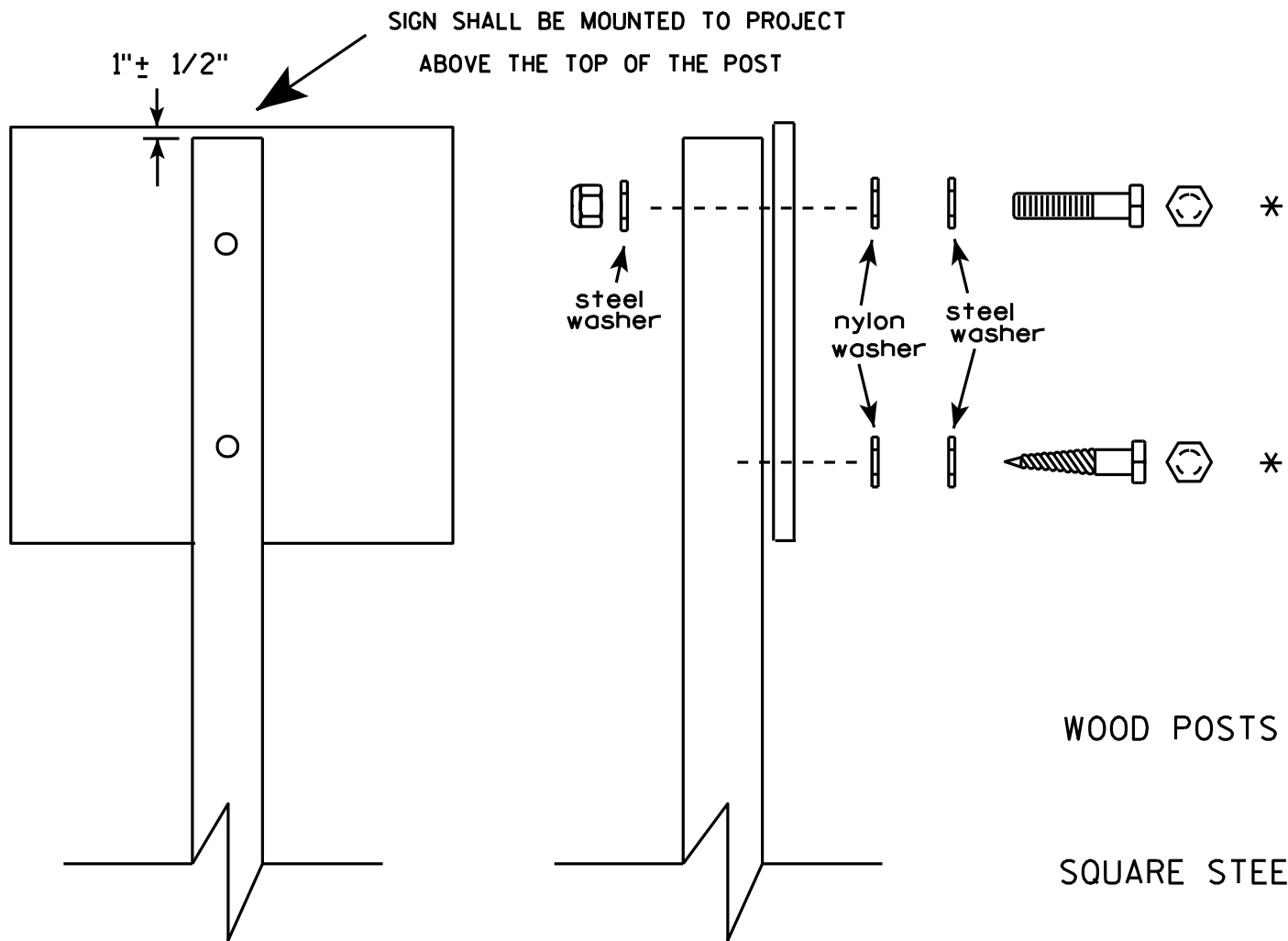
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-4.14

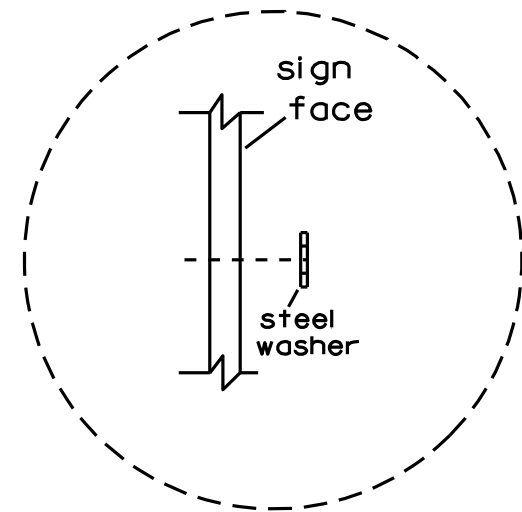


Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- WOOD POSTS (4" x 4" or 4" x 6")
LAG SCREWS - 3/8" X 3"
MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts
- SQUARE STEEL POSTS (2" x 2")
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.

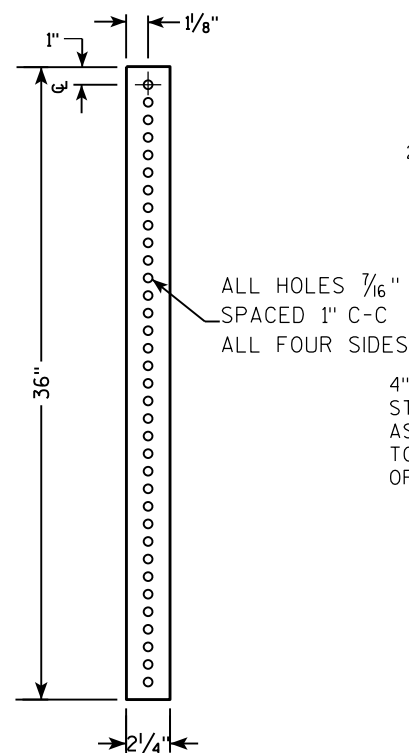


Washer Placement when Sign Has Other Than Type H or Type F Face

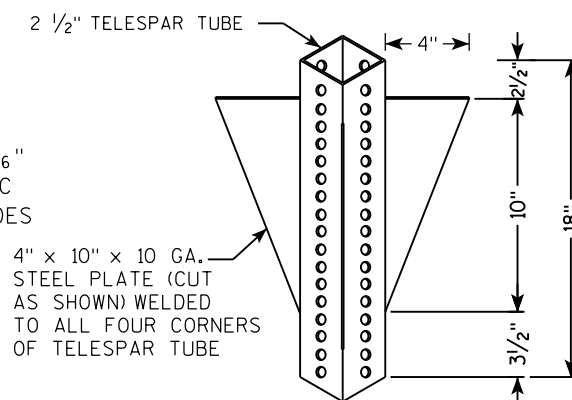
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/23/10	PLATE NO. A4-8.7

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



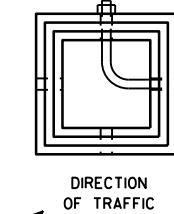
**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY:

- TELESCOPE PIECES FLUSH AT TOP**: Indicated by a dimension line on the left.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The main vertical support.
- ALL HOLES $\frac{7}{16}$ " SPACED 1" C-C ALL FOUR SIDES**: Specification for the perforations in the tubular section.
- SIGN**: Attached to the top of the post.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to a separate drawing for hardware.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Hardware used to secure the post to the base.
- 1"**: Dimension for the offset of the anchor bolt from the post face.
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT**: Hardware used to secure the base plate.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: The base plate.
- 2 1/4" SQUARE X 36"**: The main vertical post.
- Dimensions**:
 - 36" (Total height of the post section)
 - 18" (Height of the upper section)
 - 12" (Height of the lower section)
- Arrows A**: Indicate downward forces or weights applied to the post.

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT



SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

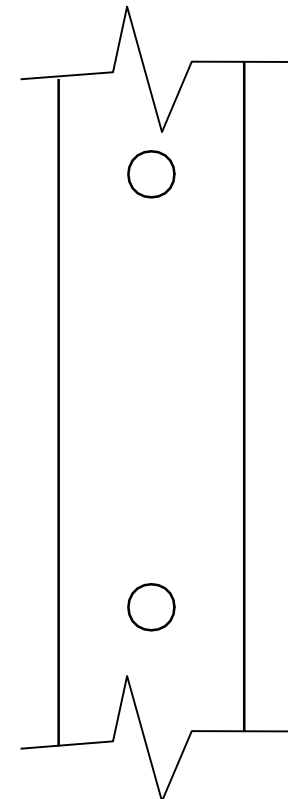
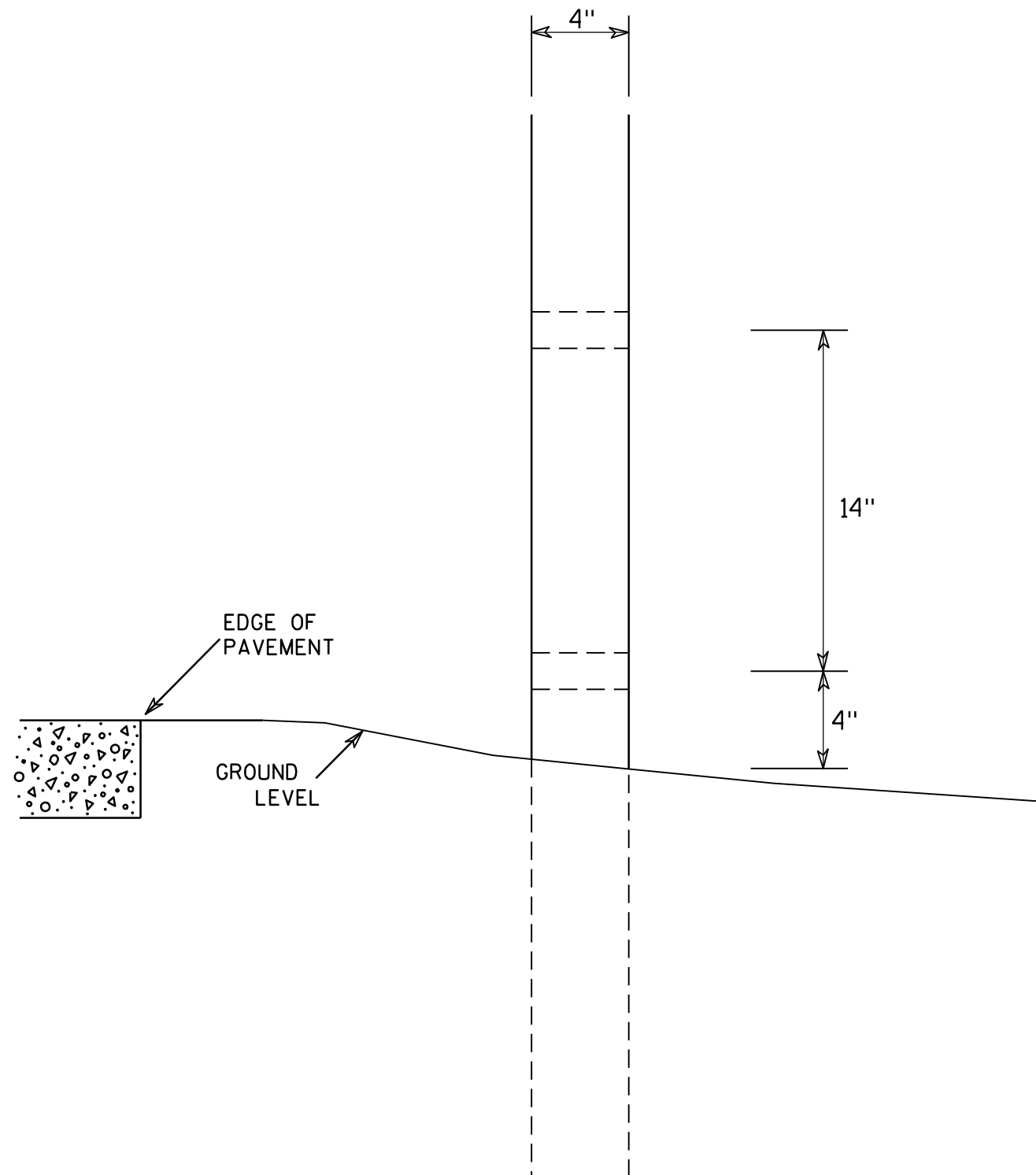
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1 1/2" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

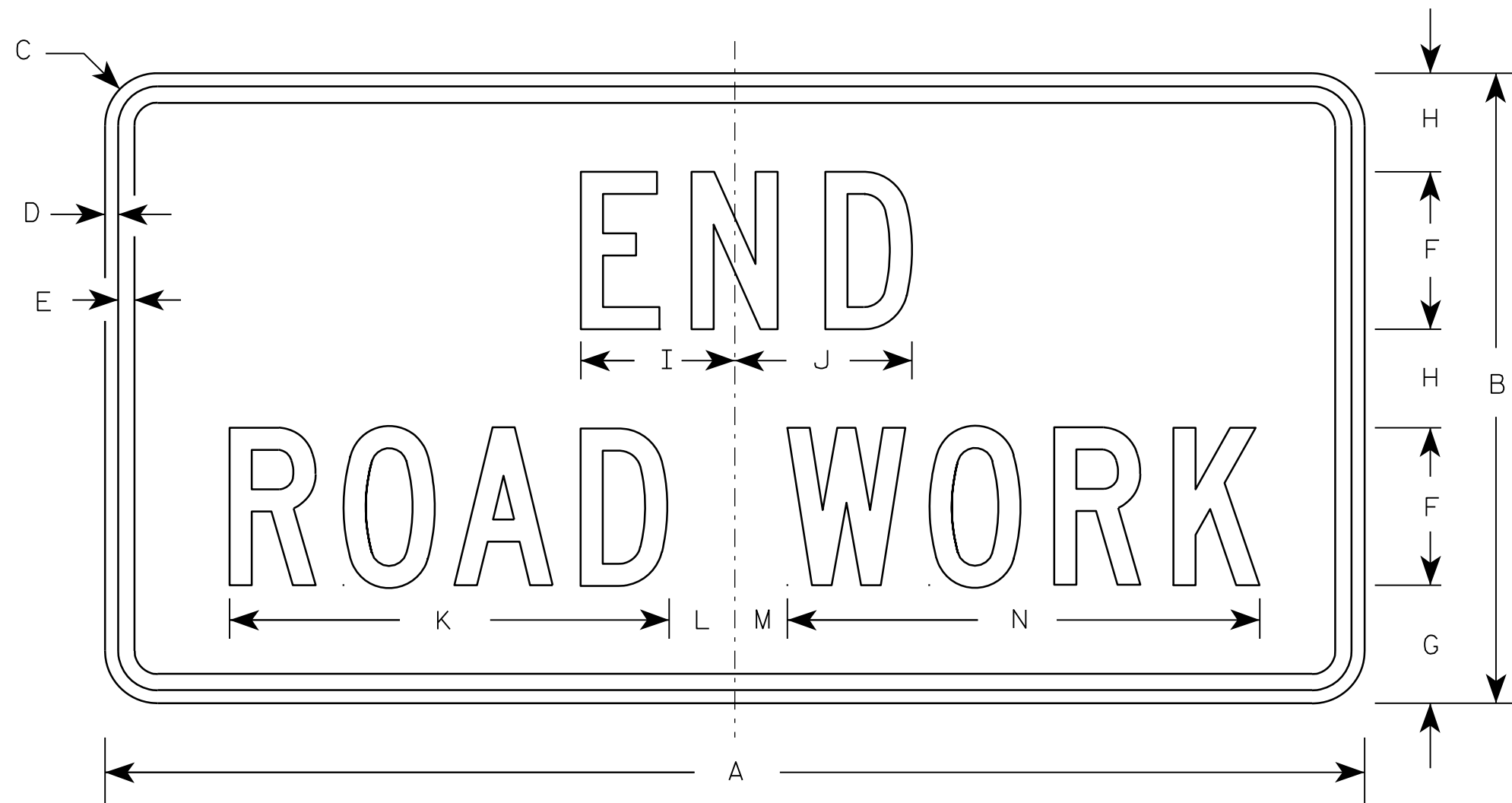
HWY:

COUNTY:

SHEET NO:

E

7



G20-2A

Metric equivalent
for this sign is:

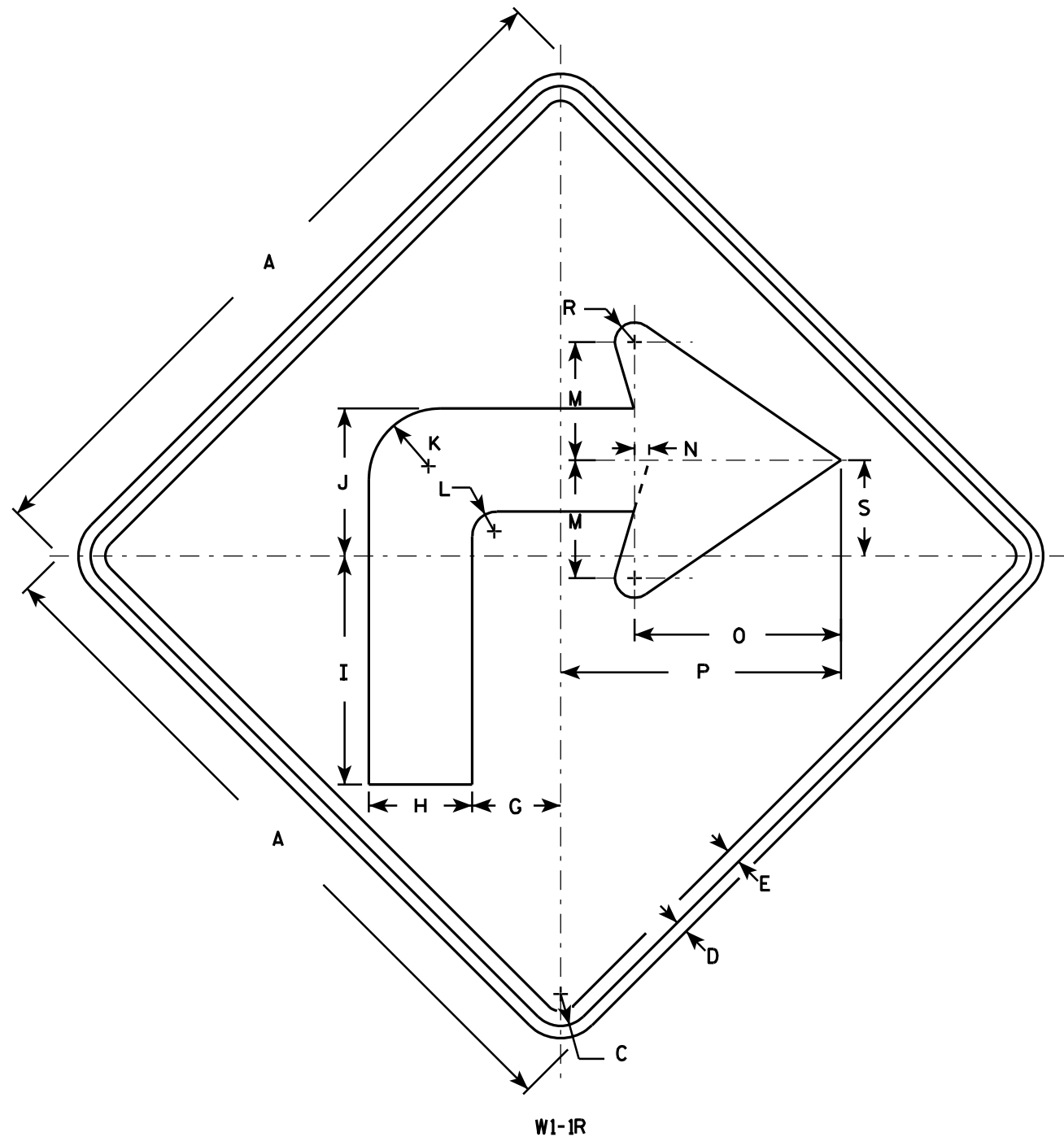
SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

NOTES

- Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Orange
Message - Black
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-1L is the same as W1-1R except the arrow is reversed along the vertical centerline.

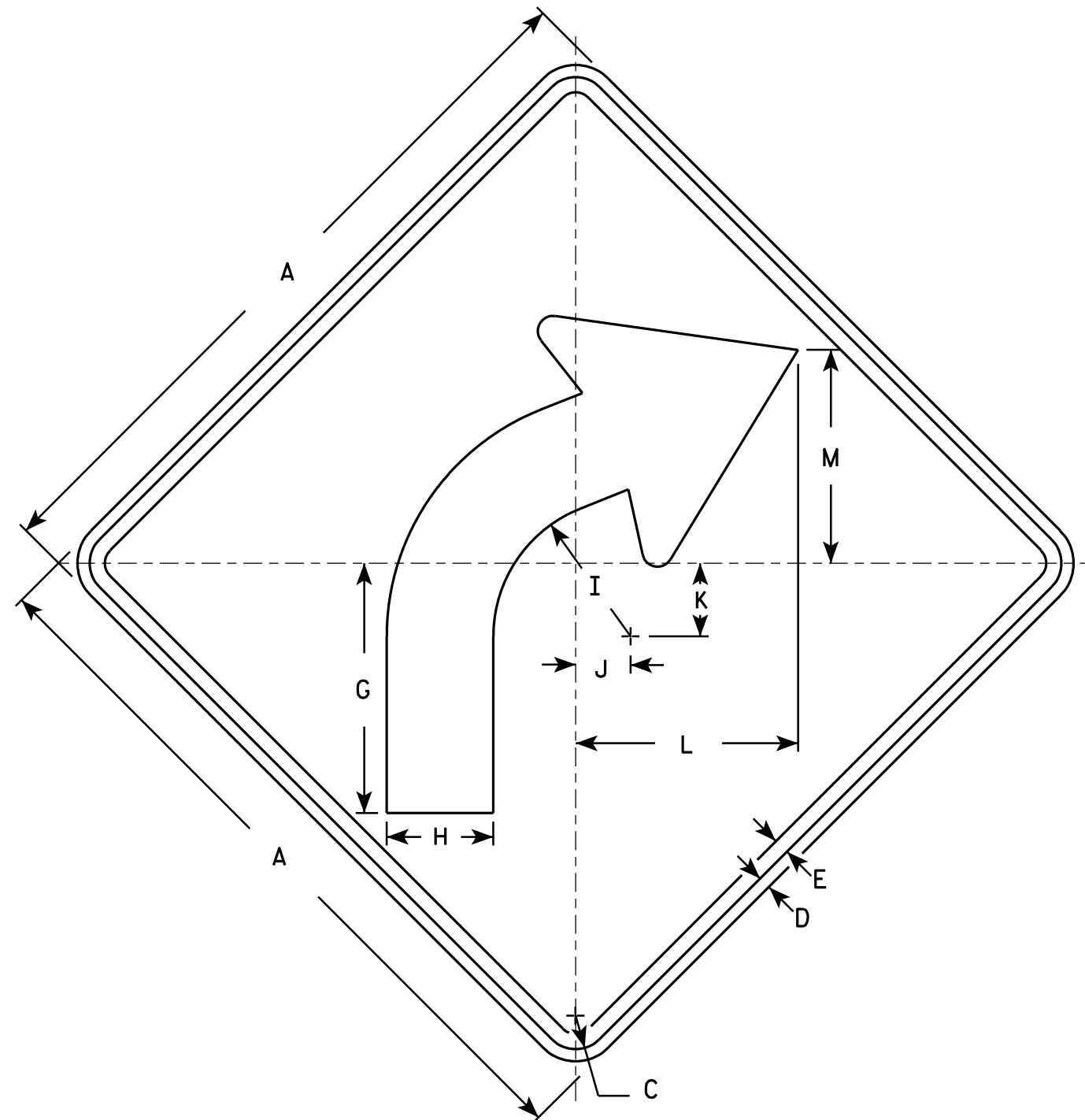
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		3	3 1/2	7 3/4	5	2 1/2	7/8	4	1/2	7	9 1/2		5/8	3 1/4								4.0
2S	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
2M	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
3	36		1 5/8	5/8	3/4		4 1/2	5 1/4	11 5/8	7 1/2	3 5/8	1 1/4	6	3/4	10 1/2	14 1/4		1	4 7/8								9.0
4	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0
5	48		2 1/4	3/4	1		6	7	15 1/2	10	4 7/8	1 5/8	8	1	14	19		1 1/4	6 1/2								16.0

STANDARD SIGN
W1-1

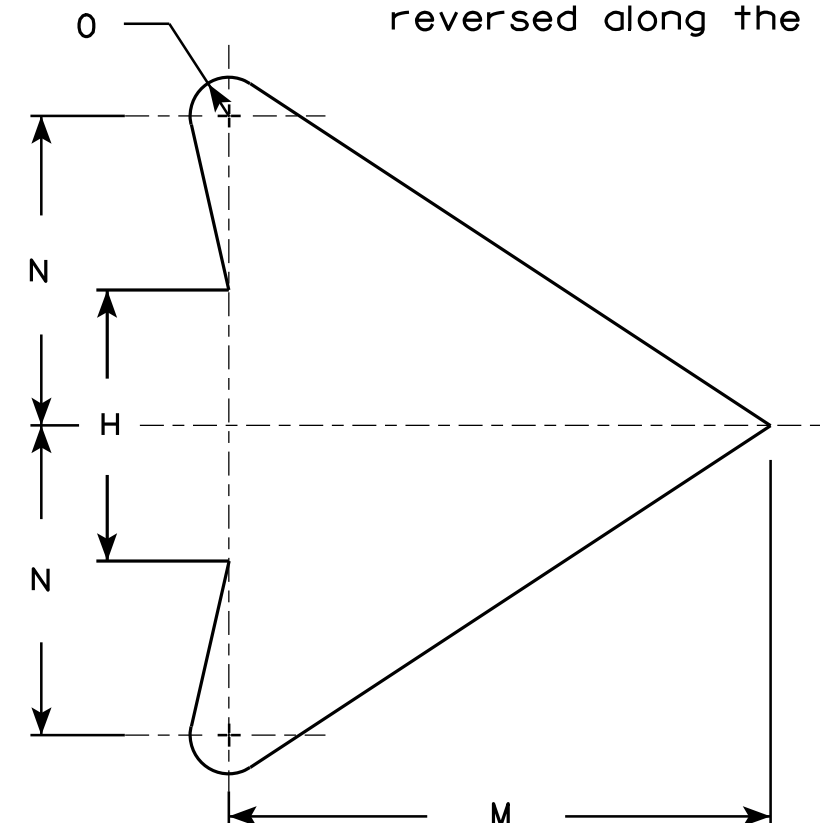
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 5/15/12 PLATE NO. W1-1.11

NOTES

- Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Yellow
Message - Black
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

STANDARD SIGN

W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10

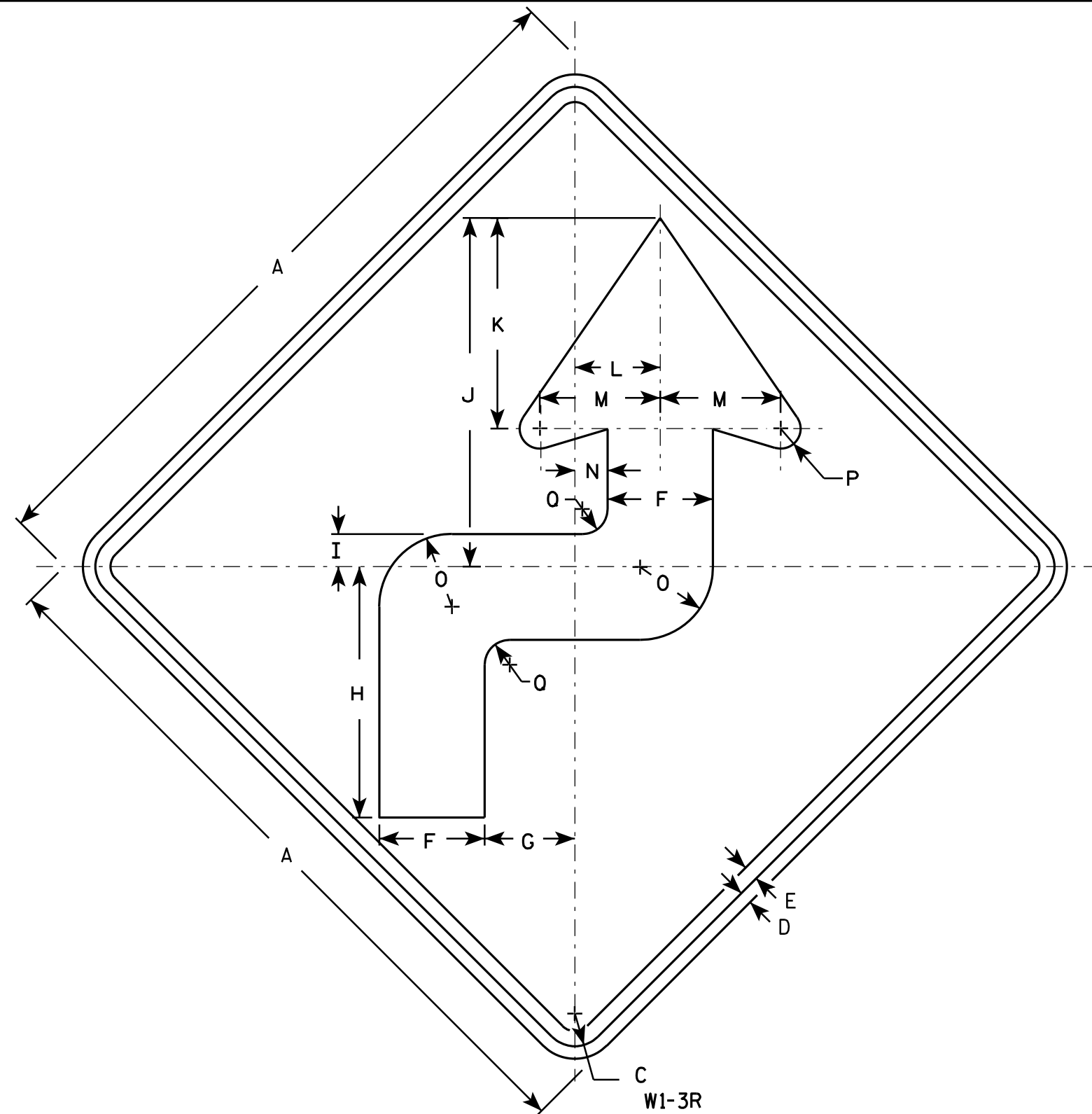
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-3L is the same as W1-3R except the arrow is reversed along the vertical centerline.

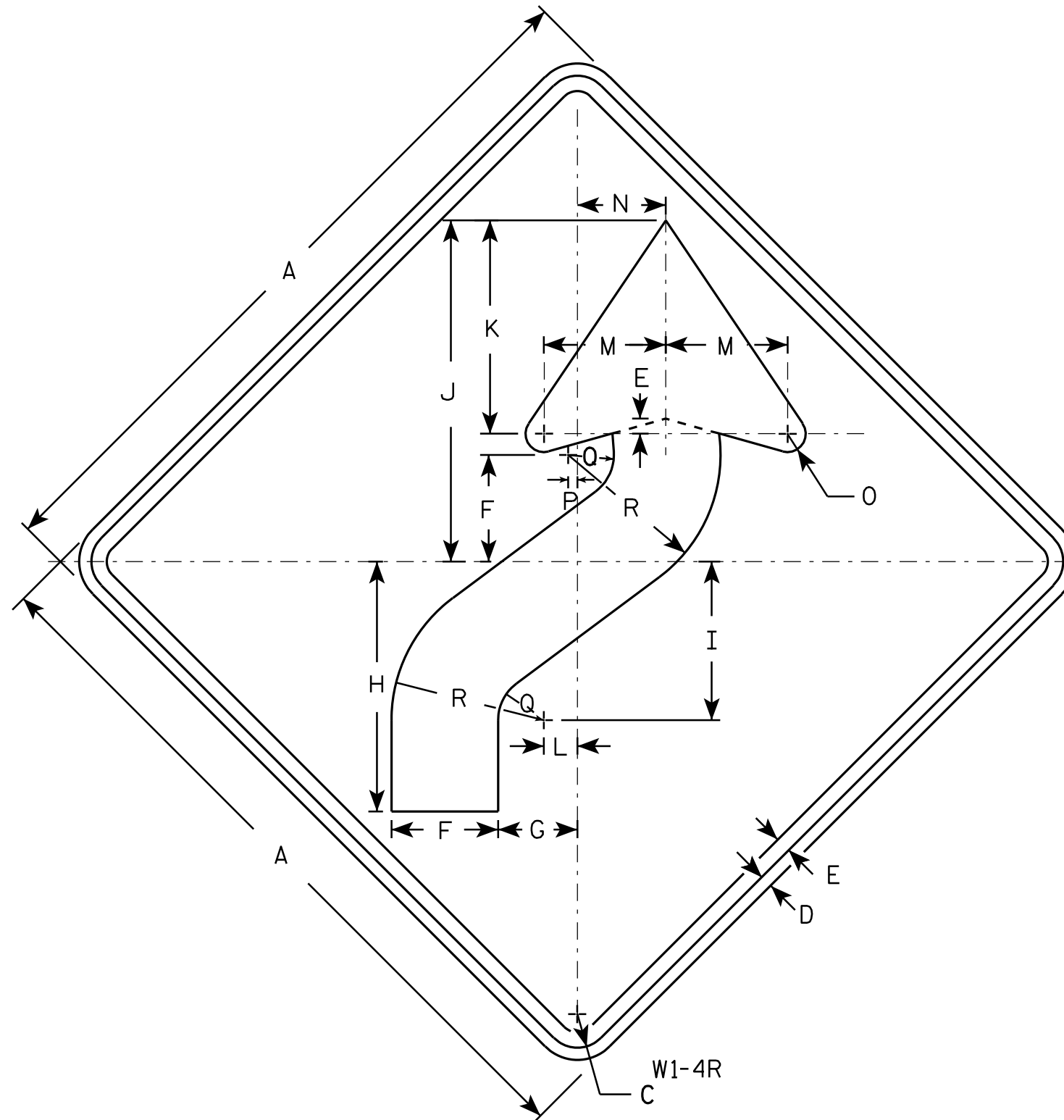
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	3	8 3/8	1 1/8	11 5/8	7	2 3/4	4	1 1/8	2 1/2	5/8	7/8										4.0
2S	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
2M	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
3	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
4	36		1 5/8	5/8	3/4	5 1/4	6	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
5	48		2 1/4	3/4	1	7	6	16 5/8	2 1/4	23 1/4	14	5 5/8	8	2 1/8	4 7/8	1 1/4	1 5/8										16.0

STANDARD SIGN W1-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 5/17/12 PLATE NO. W1-3.8

PROJECT NO: HWY: COUNTY: SHEET NO: E



NOTES

- Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- Color:
Background - Yellow
Message - Black
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- W1-4L is the same as W1-4R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	2 5/8	8 1/4	5 1/4	11 1/4	7	1 1/8	4	3	5/8	1/4	1 1/2	5									4.0
2S	30		1 3/8	1/2	5/8	4 3/8	3 1/4	10 1/4	6 1/2	14	8 3/4	1 3/8	5	3 5/8	3/4	3/8	1 7/8	6 1/4									6.25
2M	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
3	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
4	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN

W1 - 4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-4.11

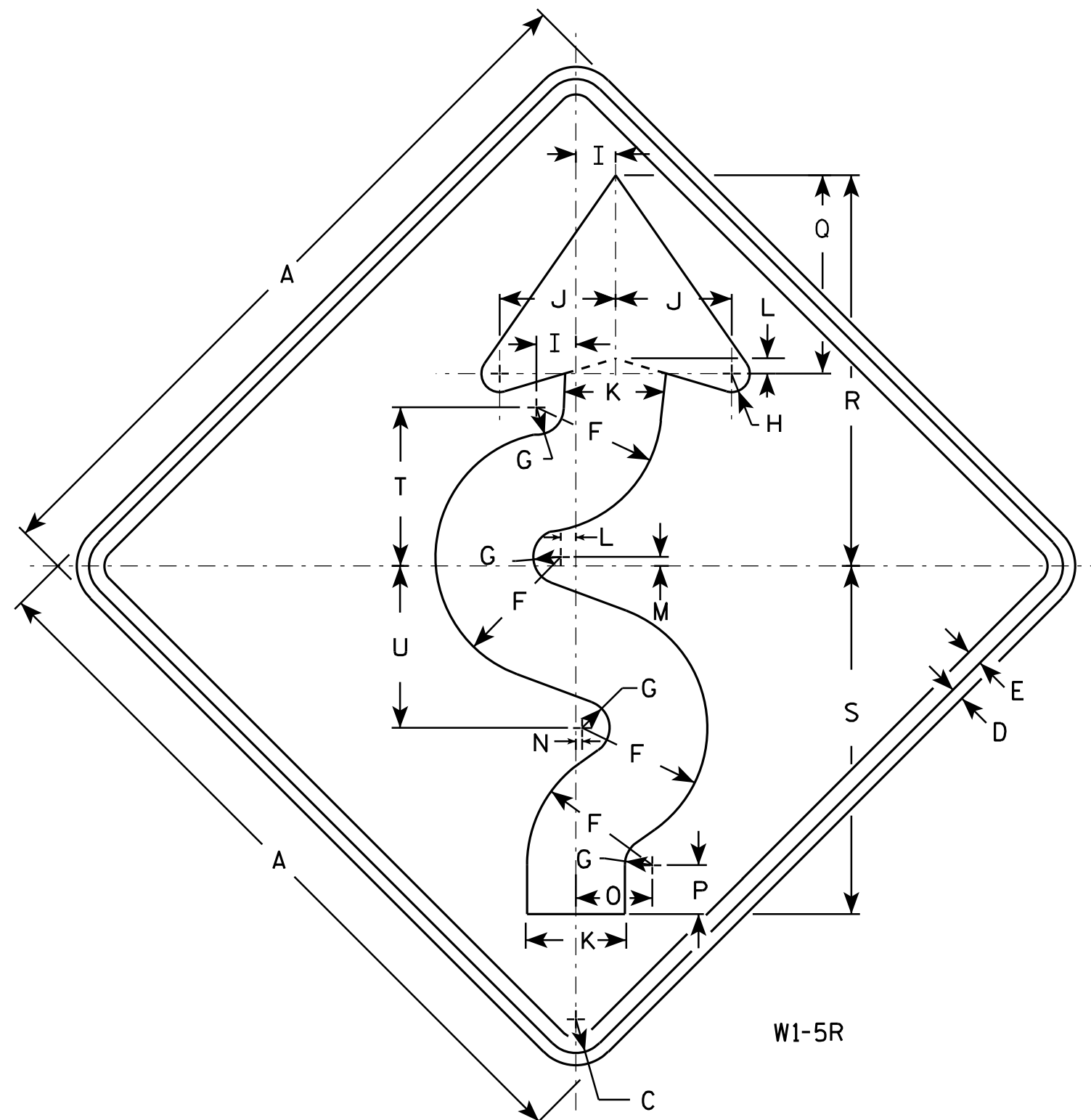
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-5L is the same as W1-5R except the arrow is reversed along the vertical centerline.

W1-5R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4 1/8	7/8	5/8	1 1/4	3 3/4	3 1/4	1/2	1/4	1/8	2 1/2	1 5/8	6 1/2	12 3/4	11 3/8	5 1/4	5 1/4						4.0
2S	30		1 3/8	1/2	5/8	5 1/8	1 1/8	3/4	1 5/8	4 3/4	4 1/8	5/8	3/8	1/4	3 1/8	2	8 1/8	16	14 1/4	6 1/2	6 5/8						6.25
2M	36		1 5/8	5/8	3/4	6 1/4	1 3/8	1	1 7/8	5 5/8	4 7/8	3/4	3/8	1/4	3 3/4	2 7/8	9 3/4	19 1/8	17 1/8	7 3/4	7 7/8						9.0
3	36		1 5/8	5/8	3/4	6 1/4	1 3/8	1	1 7/8	5 5/8	4 7/8	3/4	3/8	1/4	3 3/4	2 7/8	9 3/4	19 1/8	17 1/8	7 3/4	7 7/8						9.0
4	36		1 5/8	5/8	3/4	6 1/4	1 3/8	1	1 7/8	5 5/8	4 7/8	3/4	3/8	1/4	3 3/4	2 7/8	9 3/4	19 1/8	17 1/8	7 3/4	7 7/8						9.0
5	48		2 1/4	3/4	1	8 1/4	1 3/4	1 1/4	2 1/2	7 1/2	6 1/2	1	1/2	3/8	5	3 1/4	13	25 1/2	22 3/4	10 3/8	10 1/2						16.0

STANDARD SIGN W1-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

DATE 5/18/12

PLATE NO. W1-5.8

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

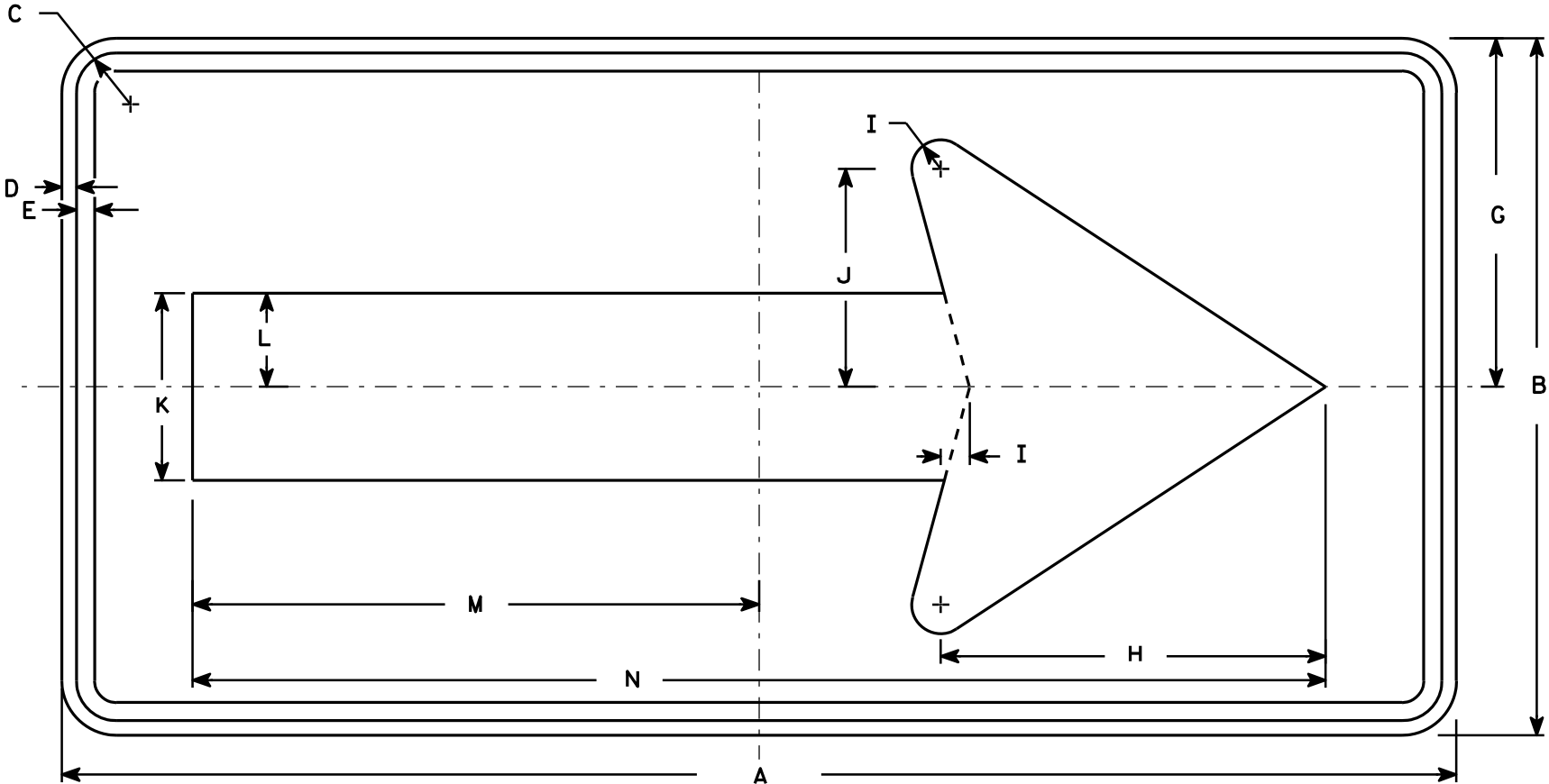
E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:

Background - Yellow

Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



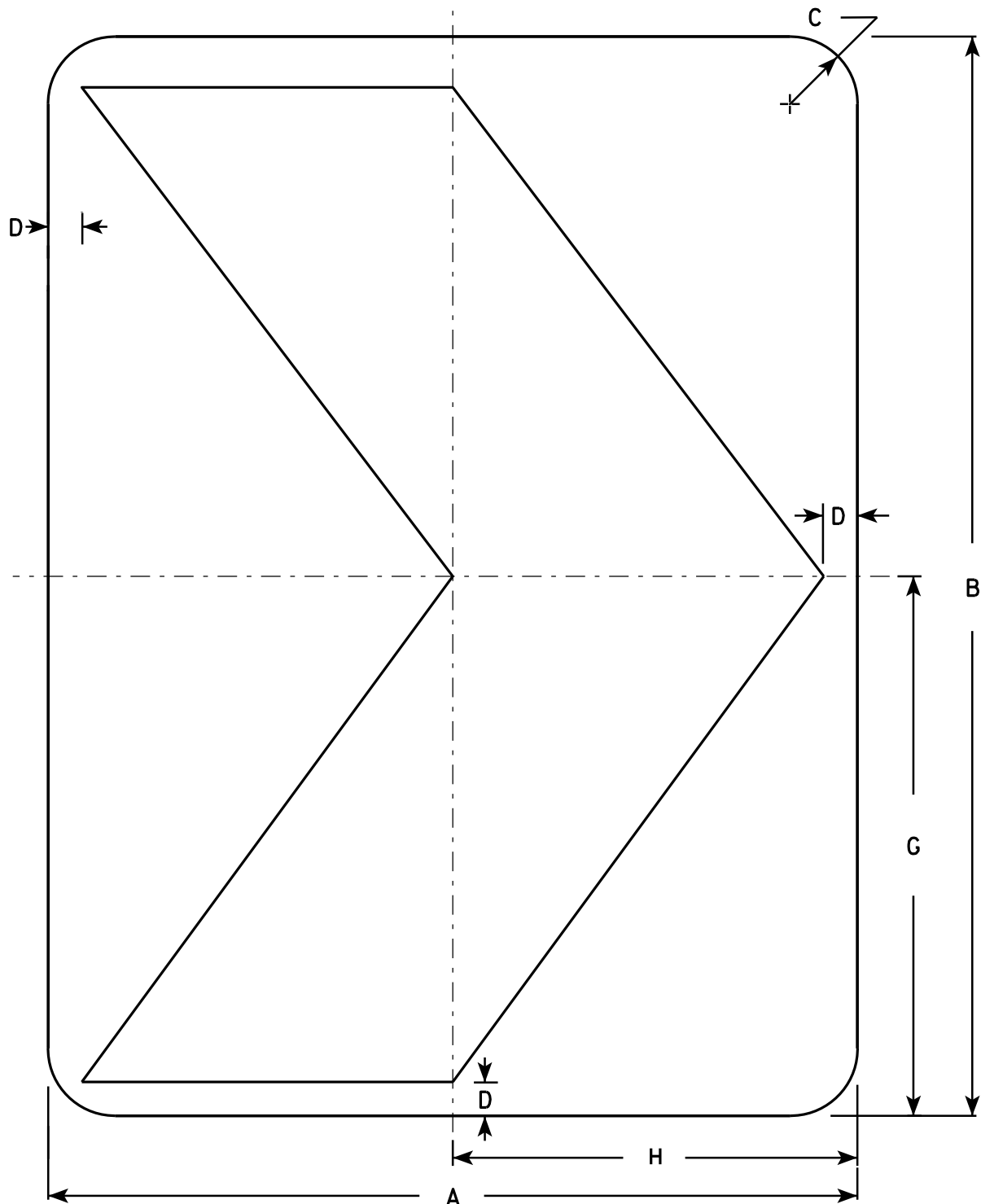
W1-6

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	3/8		9	10	3/4	5 5/8	4 3/4	2 3/8	14 5/8	29 1/4													4.5
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	96	48	2 1/4	3/4	1		24	26 1/2	2	15	13	6 1/2	39	78													32.0

STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/7/10 PLATE NO. W1-6.8



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

W1-8

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/2	1/2			9	6																			1.5
2S	18	24	1 1/2	3/4			12	9																			3.0
2M	18	24	1 1/2	3/4			12	9																			3.0
3	24	30	1 1/2	1			15	12																			5.0
4	30	36	1 7/8	1 1/4			18	15																			7.5
5	36	48	2 1/4	1 1/2			24	18																			12.0

STANDARD SIGN

W1-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-8.6

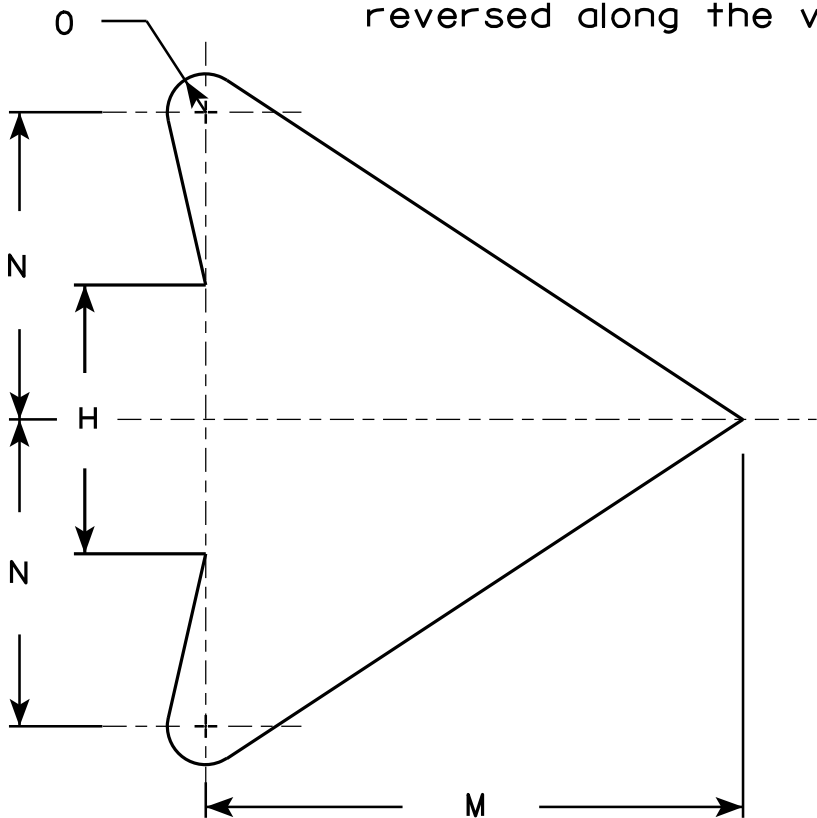
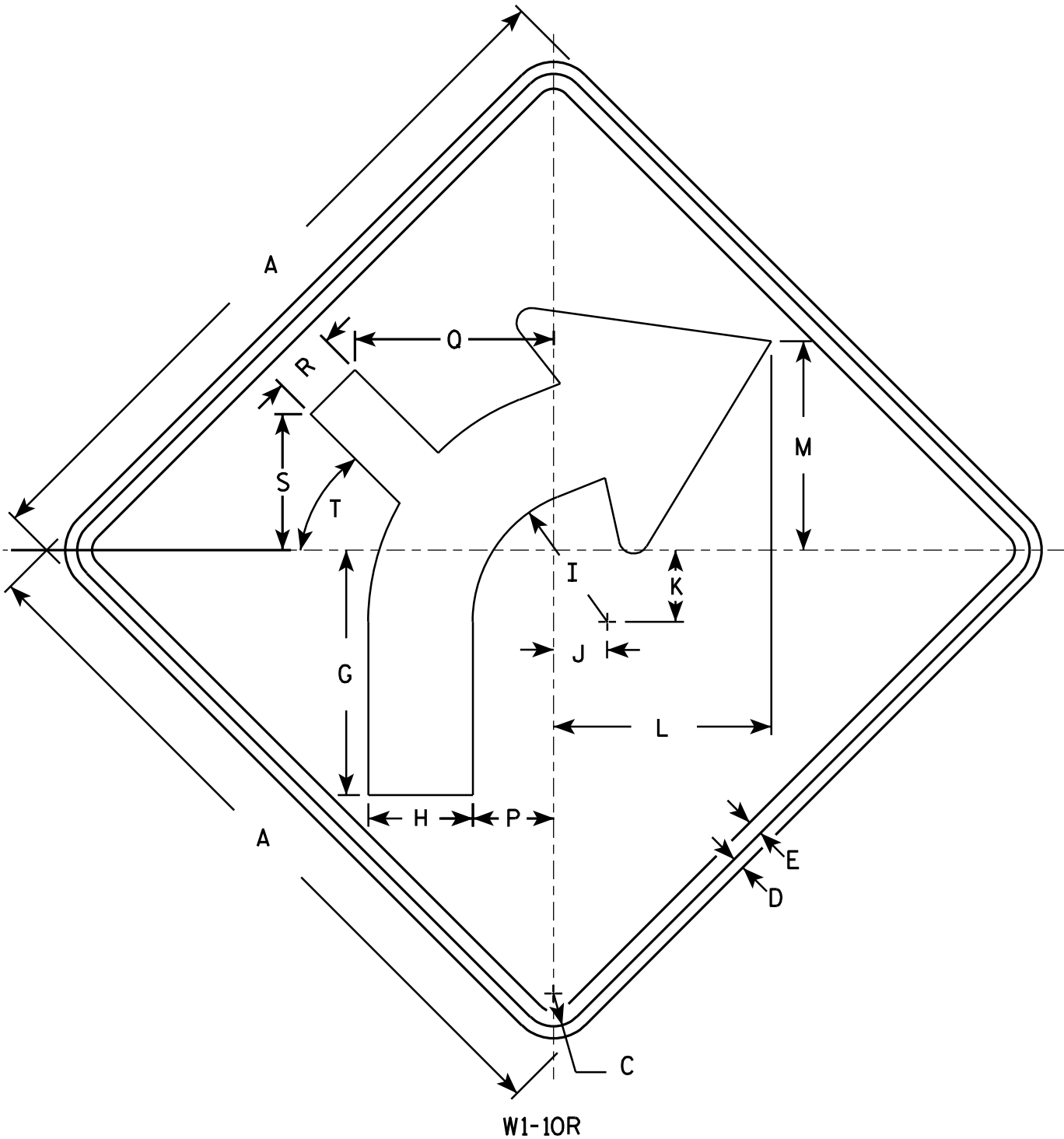
PROJECT NO:

SHEET NO:

E

NOTES

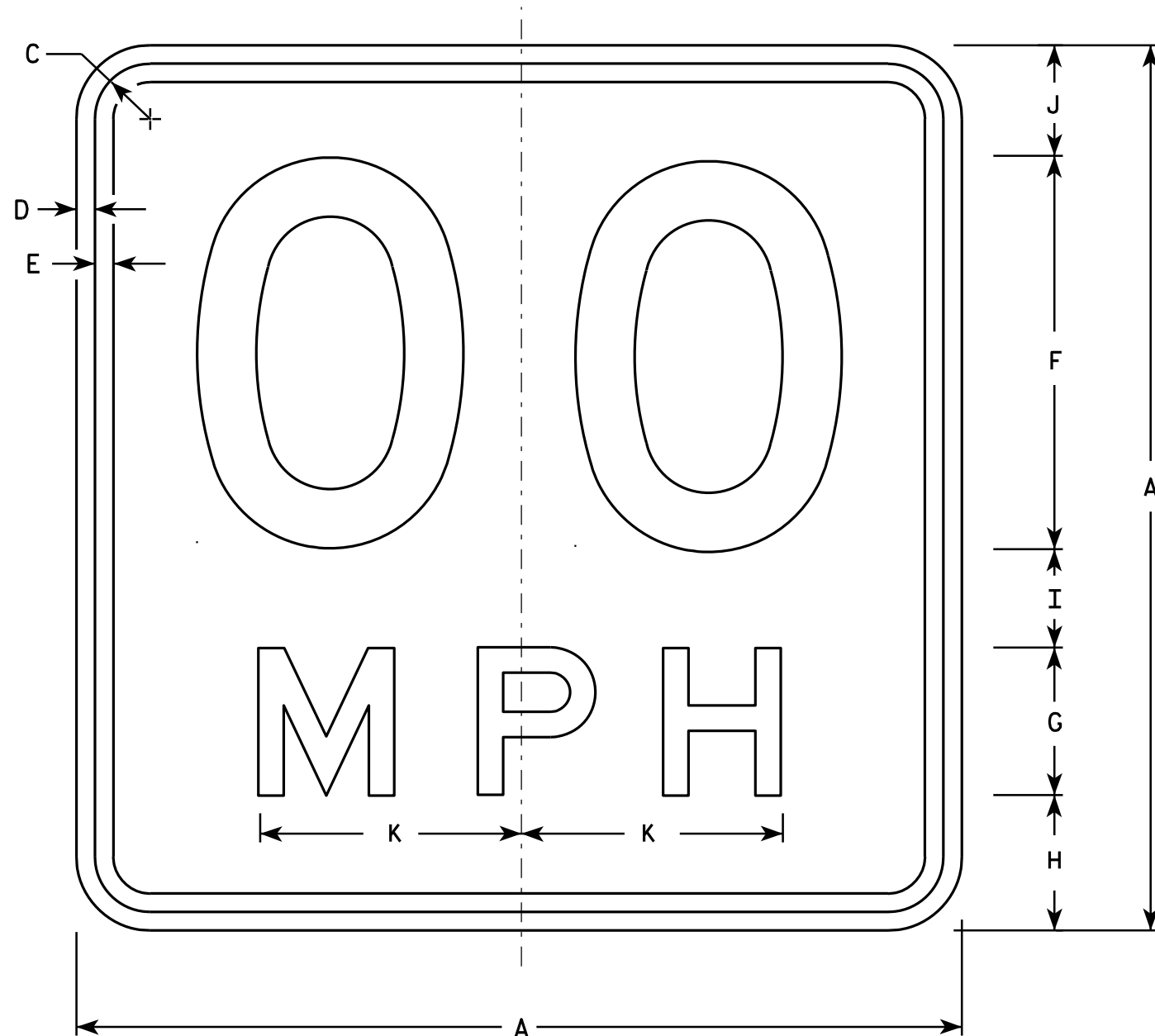
- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Yellow
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W1-10L is the same as W1-10R except the arrow is reversed along the vertical centerline.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2	2 5/8	6 5/8	2 1/8	4 1/2	45°							4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8	3 3/8	8 3/8	2 5/8	5 3/4	45°							6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4	4	10	3 1/4	6 7/8	45°							9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4	4	10	3 1/4	6 7/8	45°							9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4	4	10	3 1/4	6 7/8	45°							9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1	5 3/8	13 1/4	4 1/4	9 1/8	45°							16.0

STANDARD SIGN
W1-10

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 5/17/12 PLATE NO. W1-10.3



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

W13-1

- * For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

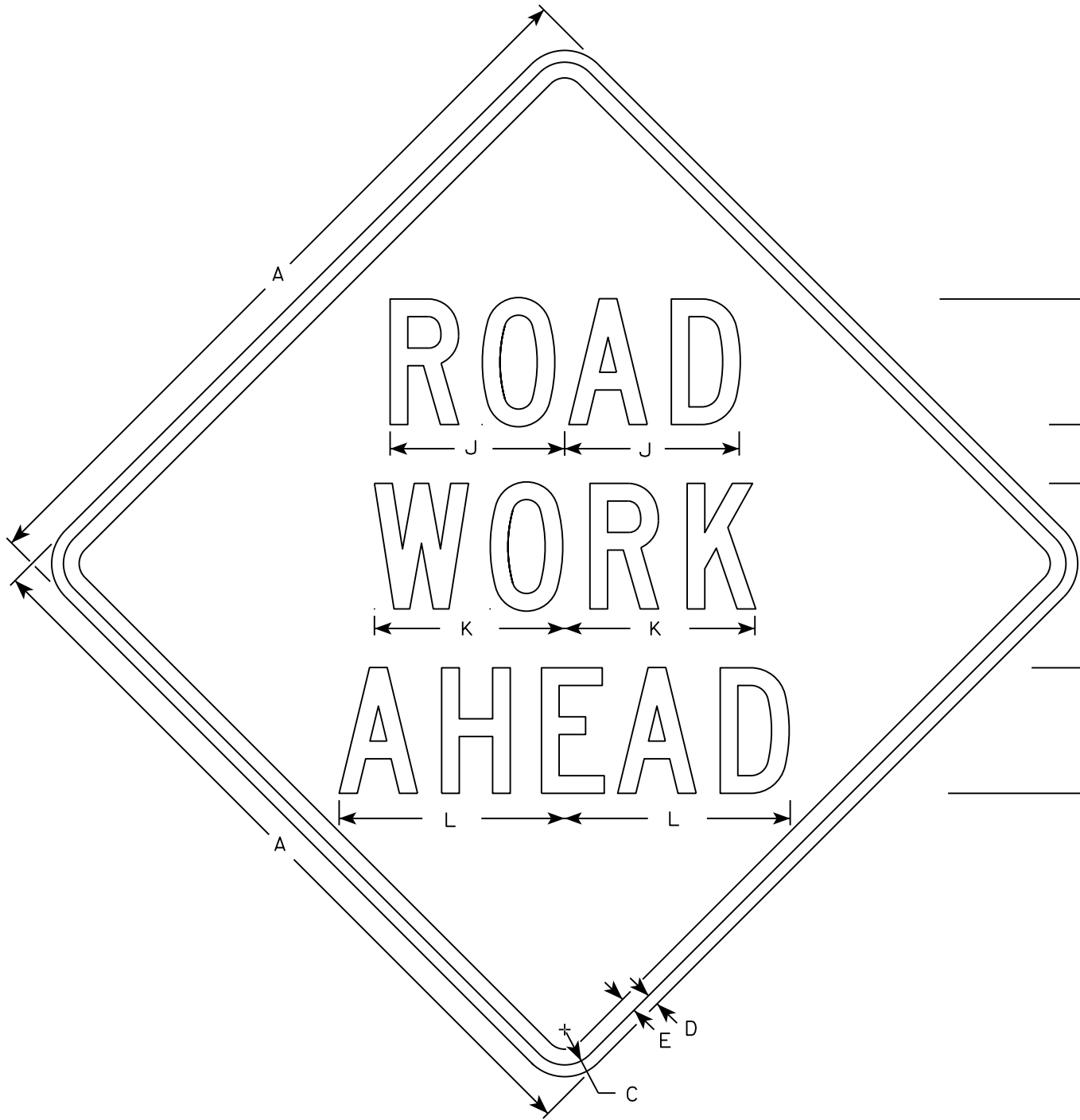
PROJECT NO:

HWY:

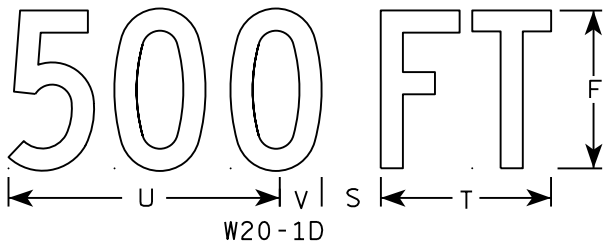
COUNTY:

SHEET NO:

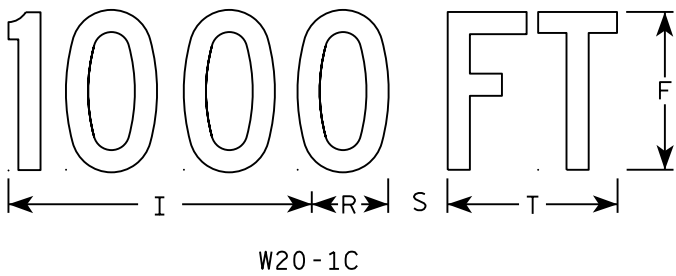
E



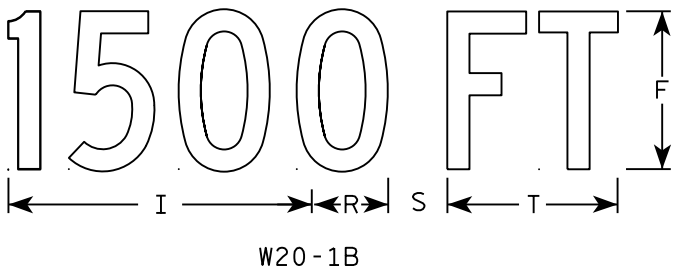
W20-1A



W20-1D



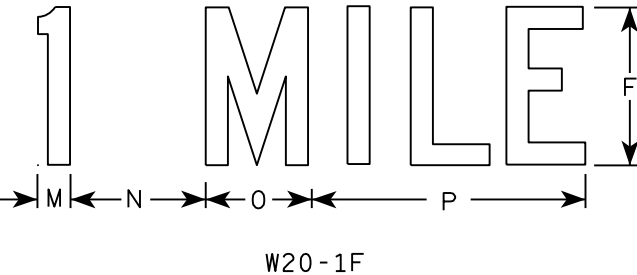
W20-1C



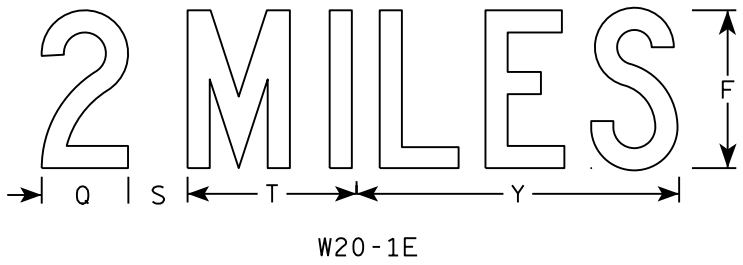
W20-1B



W20-1G



W20-1F



W20-1E

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

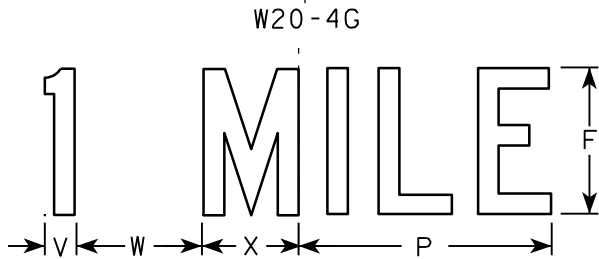
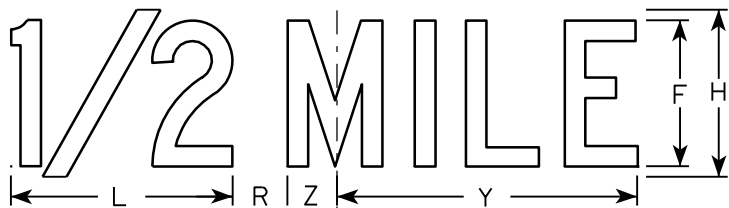
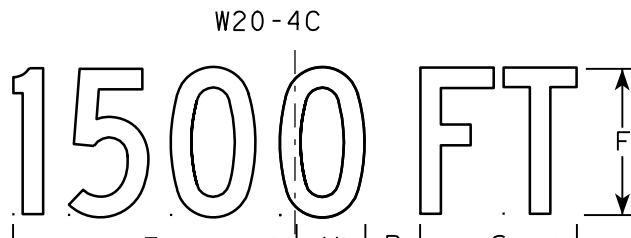
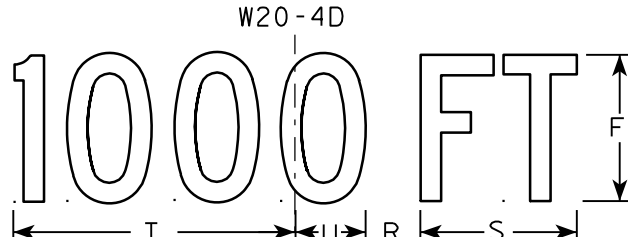
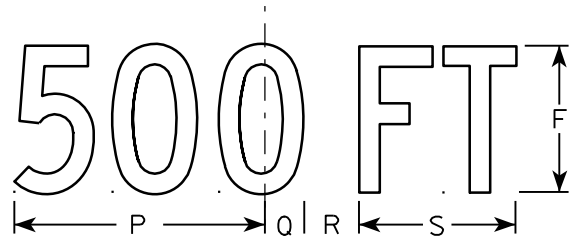
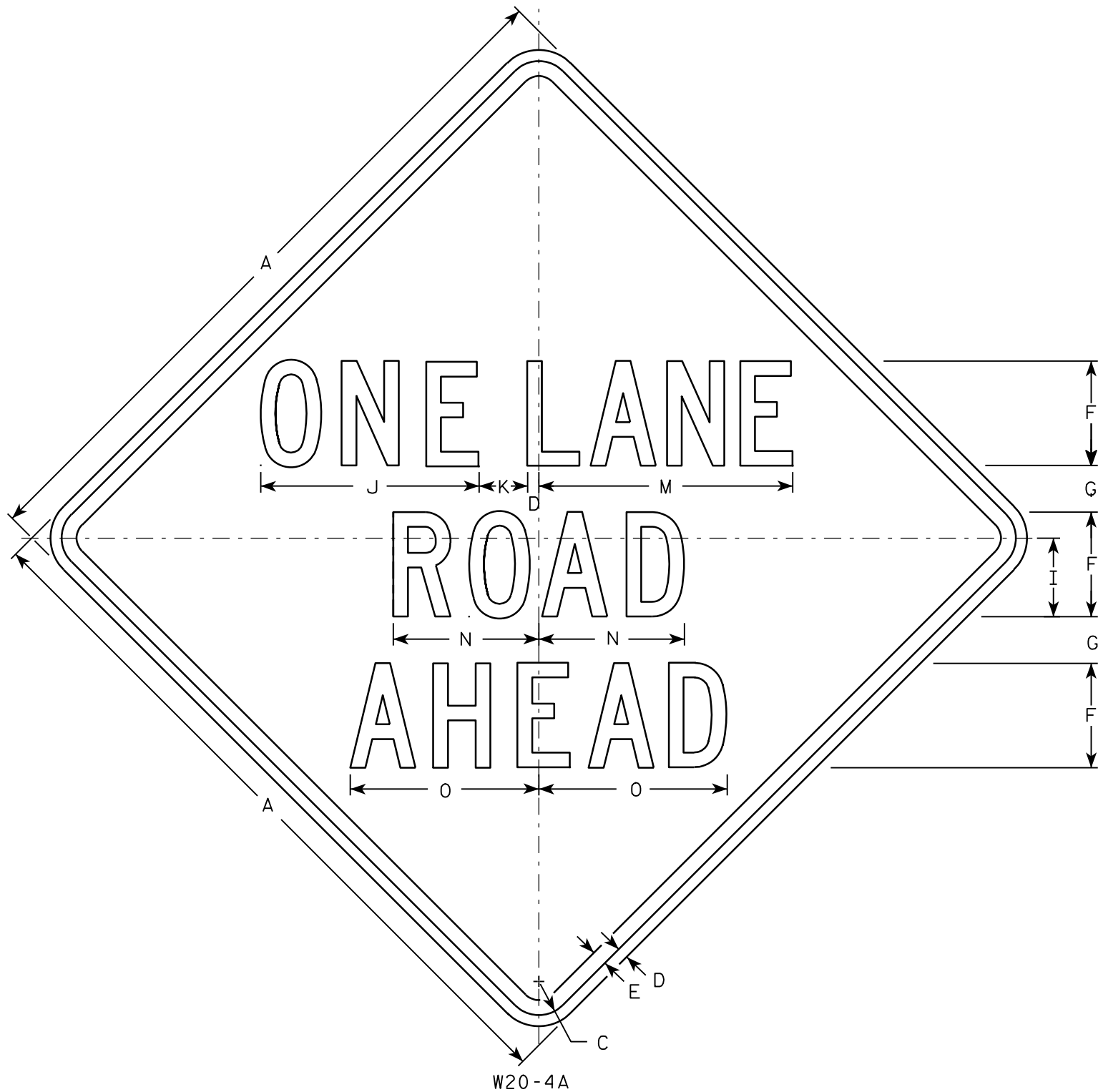
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/07/15 PLATE NO. W20-1.10



NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

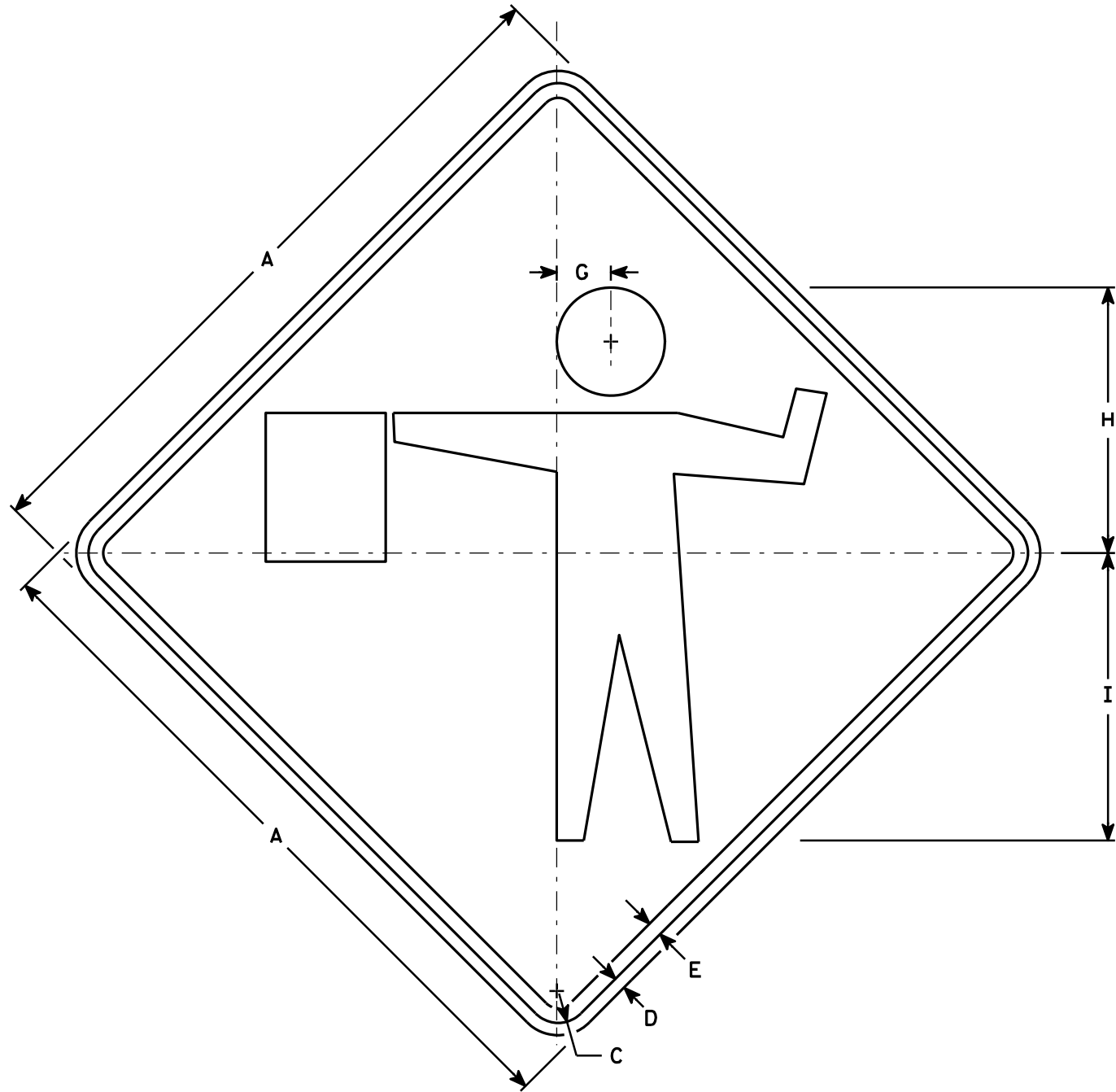
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 3/8	6	3 3/4	10 3/8	2 3/8	8	13 1/2	7	8 7/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-4A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-4.9



W20-7A

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:
Background - Orange
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4		2 3/4	13 1/2	14 5/8																		9.00
2S	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
2M	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
3	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
4	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
5	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00

STANDARD SIGN
W20-7A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-7A.5

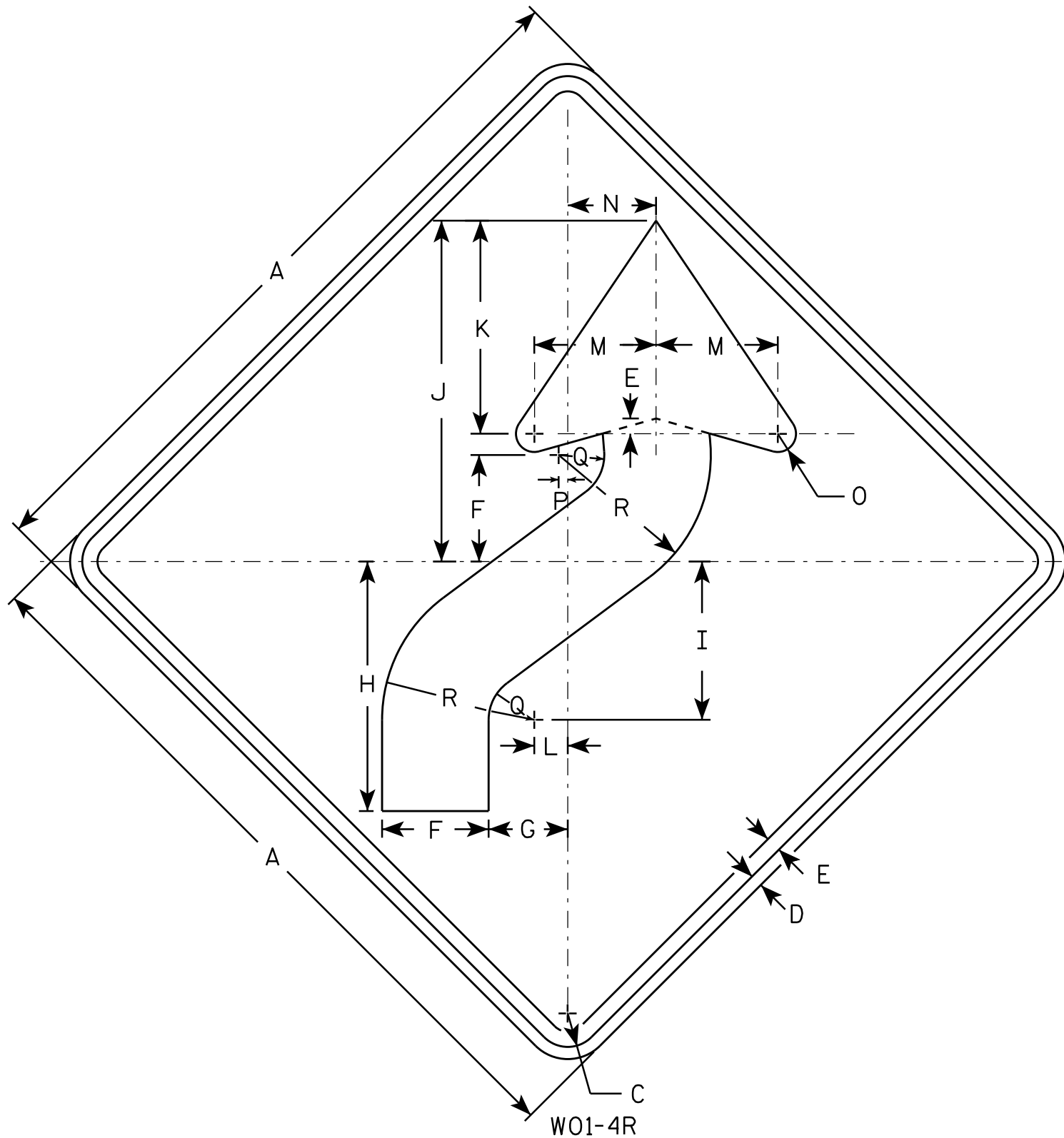
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN W01-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-4.1

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

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