WIS

0 -44-66

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MAY	2016
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ORDER OF SHEETS

Section No. 1 Section No. 3 Estimate of Quantities Section No. 3 Miscellaneous Quantitles

Plan and Profile Section No. 5 Standard Detail Drawings Section No. 6

Section No. 7 Sign Plates

Section No. 9 Cross Sections

TOTAL SHEETS = 134

DESIGN DESIGNATION

CONVENTIONAL SYMBOLS

LIMITED HIGHWAY EASEMENT

PROPOSED OR NEW R/W LINE

EXISTING RIGHT OF WAY

SLOPE INTERCEPT

REFERENCE LINE

EXISTING CULVERT

PROPOSED CULVERT

COMBUSTIBLE FLUIDS

WOODED OR SHRUB AREA

MARSH AREA

CORPORATE LIMITS

PROPERTY LINE

LOT LINE

N/A = N/A

= N/A

= N/A = N/A

= N/A

= IDL

A.A.D.T.

A.A.D.T. D.H.V.

DESIGN SPEED

D.D.

ESALS

PLAN

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION Typical Sections and Details

PLAN OF PROPOSED IMPROVEMENT

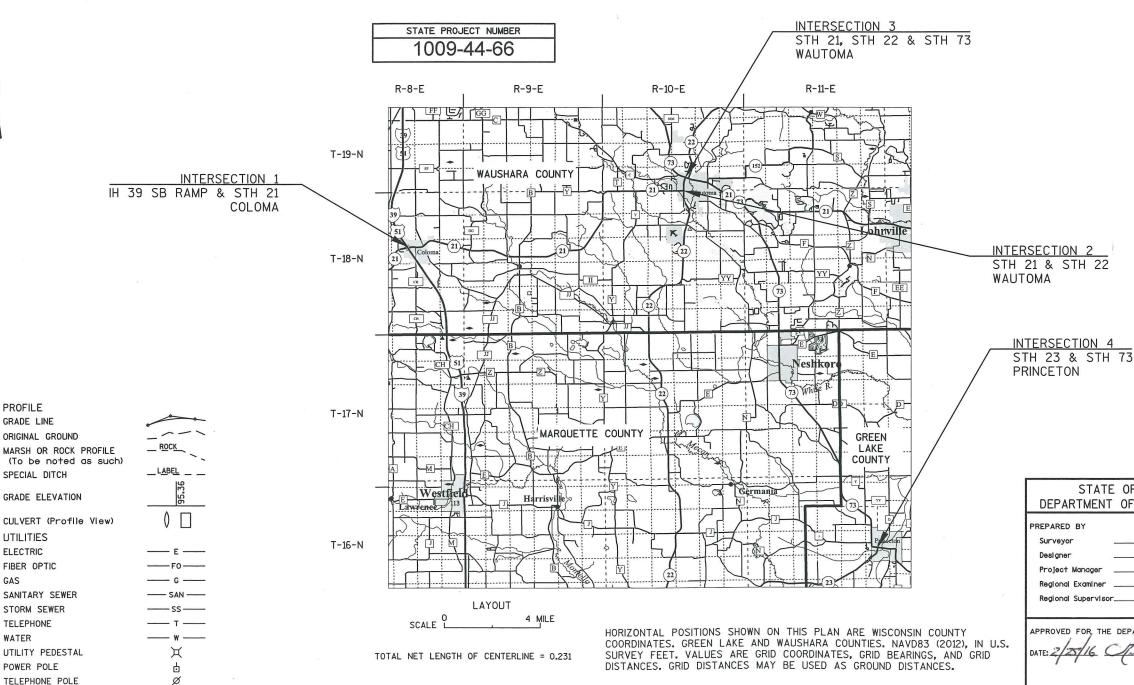
FEDERAL PROJECT STATE PROJECT **PROJECT** CONTRACT 1009-44-66

NC REGION INTERSECTION IMPROVEMENTS

REGIONWIDE OSOW MITIGATION ROUTES

VARIOUS HIGHWAYS

NORTH CENTRAL REGION WIDE



FILE NAME: N:\PDS\C3D\10094465\SHEETSPLAN\010101_TI.DWG

PLOT DATE: 2/25/2016 1:33 PM

PLOT BY : GRULING, ZACHARY P PLOT NAME :

WISDOT/CADDS SHEET 10

E

REGION

ZACHARY GRULING

MARK STEIDL

CHERYL SIMON

SHANNON RILEY

STATE OF WISCONSIN

DEPARTMENT OF TRANSPORTATION

PREPARED BY

Project Manager

Regional Examiner

Surveyor

Designer

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

PURSUANT TO CHAPTER 59 OF THE WISCONSIN STATUTES. THE CONTRACTOR SHALL CAREFULLY MAKE A SEARCH FOR EVIDENCE OF A LANDMARK IN ALL AREAS WHERE SUCH A LANDMARK MAY EXIST.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

ENVIRONMENTAL CONTACTS

WI Dept. of Natural Resources

427 E Tower Drive Suite 100 Wautoma, WI 54982 Attn: Bobbi Jo Fischer Phone: (920) 787-3015 bobbi.fischer@wisconsin.gov

UTILITY CONTACTS

Alliant Energy - Electricity

Mark Villars 880 N Wisconsin Street Berlin, WI 54923 (920) 361-5652

(920) 290-0827 (Mobile) markvillars@alliantenergy.com

Charter Communications - Communication Line

Tony Klatt N3760 County Road DJ Juneau. WI 53039 (920) 263-0062 tony.klatt@charter.net

City of Princeton - Sewer

Lee Williams 531 S Fulton Street Princeton, WI 54968 (920) 295-6612 (920) 299-6037 (Mobile)

City of Wautoma - Water

Water Utility On Call Person 210 E Main Street Wautoma, WI 54982 (920) 787-4034 (920) 765-1034 (Mobile On Call Person) wautomawater@centurytel.net

We Energies - Gas/Petroleum

Travis Kahl 1921 8th Street South Wisconsin Rapids, WI 54494 (715) 421-7256 (715) 498-6180 (Mobile) travis.kahl@we-energies.com **Century Link - Communication Line**

Tim Kroeze 201 Stark Street Randolph, WI 53956 (920) 326-2224 (920) 219-0112 (Mobile) tim.kroeze@centurylink.com

City of Princeton - Electricity Transmission

Lee Williams 531 S Fulton Street Princeton, WI 54968 (920) 295-6612 (920) 299-6037 (Mobile)

City of Princeton - Water

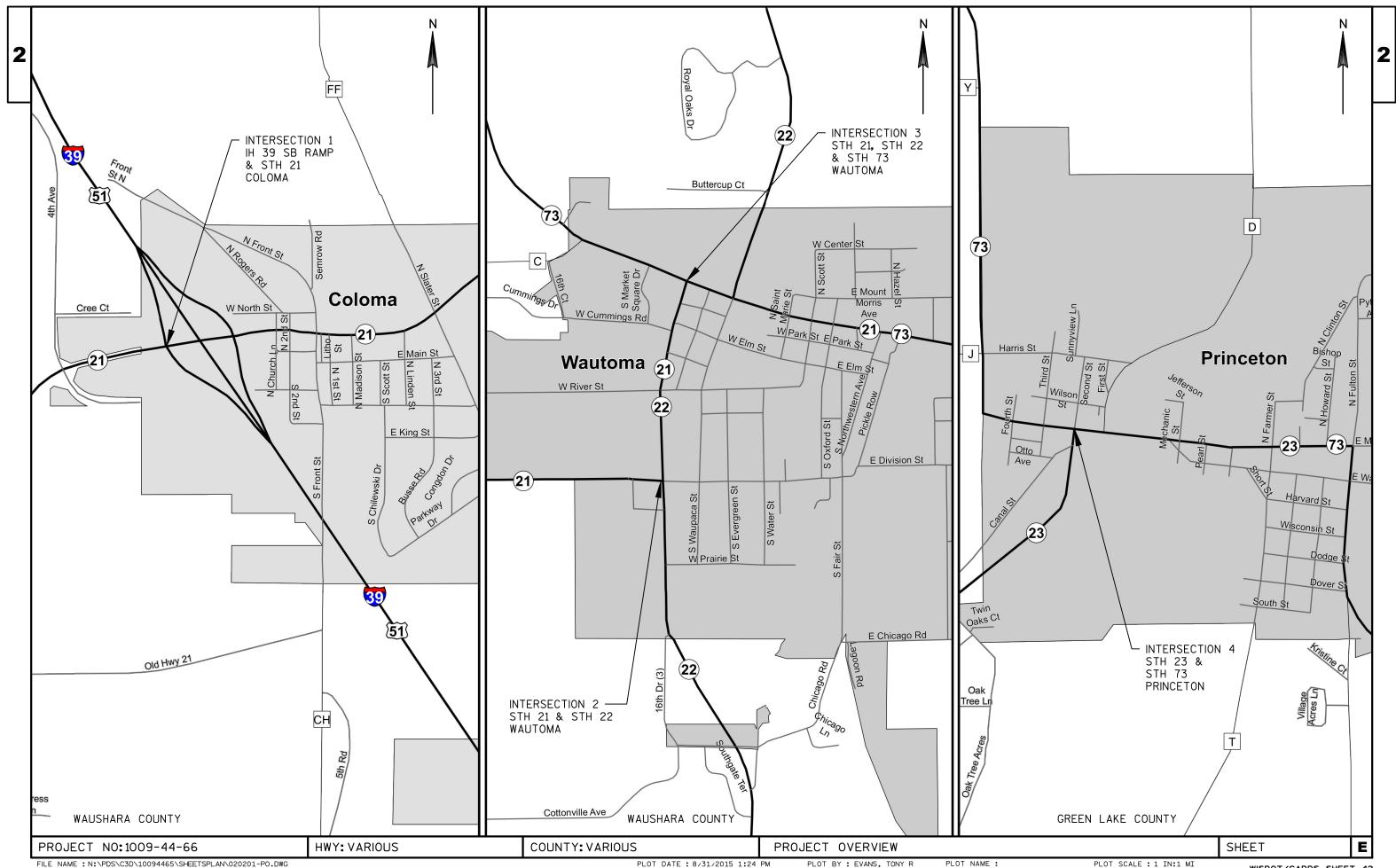
Ernest Schmidt 531 S Fulton Street Princeton, WI 54968 (920) 295-6612 (920) 299-6038 (Mobile) cityofprinceton@centurytel.net

City of Wautoma - Sewer

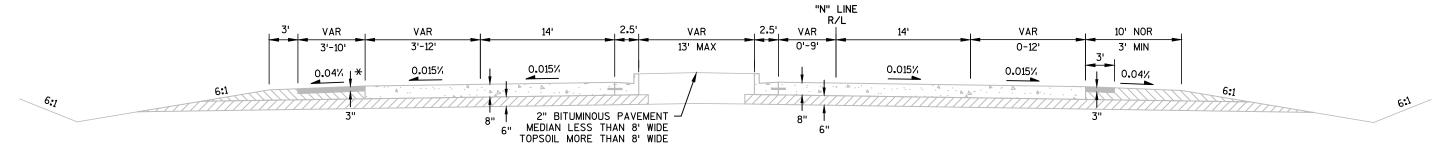
Sewer Utility On Call Person 210 E Main Street Wautoma, WI 54982 (920) 787-4034 (920) 765-1034 (Mobile On Call Person)) wautomawater@centurytel.net



Ε PROJECT NO: 1009-44-66 **HWY: VARIOUS COUNTY: VARIOUS GENERAL NOTES** SHEET:

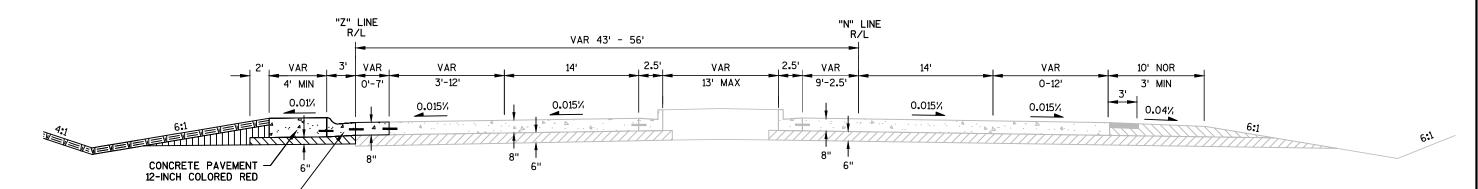






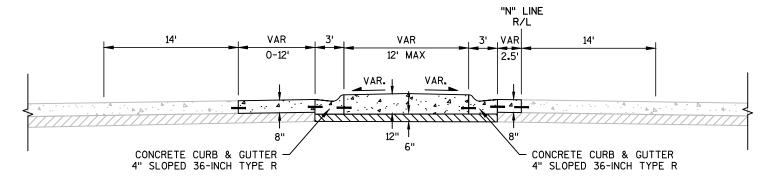
* INTERSECTION CURB BEGINS AT STA 194+62"N"

EXISTING TYPICAL SECTION STH 21
STA 193+85"N" - STA 195+31"N"



PROPOSED TYPICAL SECTION STH 21

STA 193+85"N" - STA 194+81"N
STA 0+22"Z" - STA 1+20"Z"



LEGEND

EXISTING HMA PAVEMENT

EXISTING BASE AGGREGATE DENSE 3/4-INCH

CONCRETE CURB & GUTTER 4" SLOPED 36-INCH TYPE R

EXISTING BASE AGGREGATE DENSE 1 1/4-INCH

EXISTING PCC PAVEMENT

PROPOSED PCC PAVEMENT

PROPOSED BASE AGGREGATE DENSE 3/4-INCH

FILL

■W■W■ 6" TOPSOIL, SEED, FERTILIZE AND MULCH

PROPOSED TYPICAL SECTION STH 21

MEDIAN ISLAND

STA 194+81"N" - STA 195+31"N"

PROJECT NO:1009-44-66 HWY: VARIOUS COUNTY: VARIOUS TYPICAL SECTION - STH 21 & IH 39 RAMP - INTERSECTION 1 SHEET

FILE NAME: N:\PDS\C3D\10094465\SHEETSPLAN\020301-TS.DWG

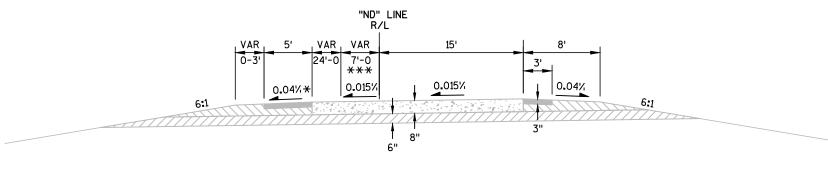
PLOT DATE: 1/25/2016 4:09 PM

PLOT BY : EVANS, TONY R

PLOT NAME :

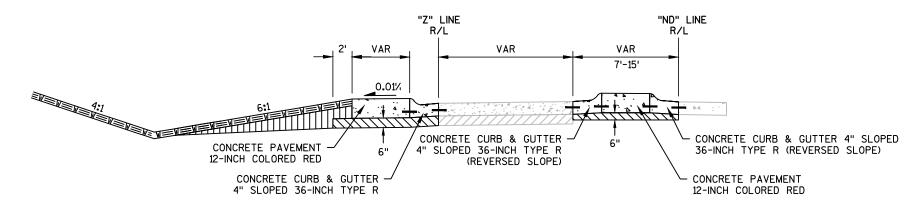
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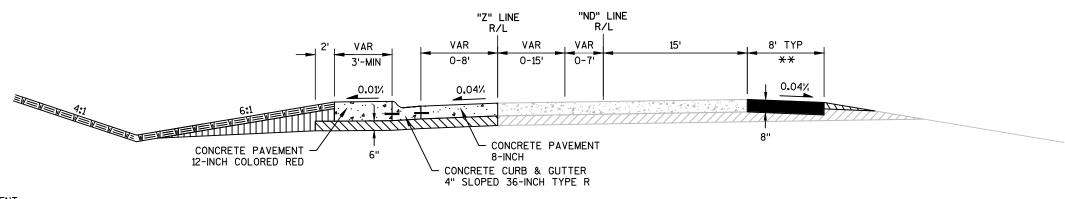
* INTERSECTION CURB ENDS AT STA 18+57"ND" *** SPLITTER ISLAND STA 17+97"ND" TO STA 18+35"ND" 7'-15' WIDE

EXISTING TYPICAL IH 39 SB RAMP SECTION STA 17+97"ND" - STA 20+12"ND"



PROPOSED STH 21/IH 39 SB OFF RAMP

STA 17+97"ND" - STA 18+35"ND" (ISLAND) STA 1+20"Z" - STA 1+56"Z" (OUTSIDE RADIUS)



PROPOSED TYPICAL IH 39 SB RAMP SECTION

STA 18+35"ND" - STA 20+12"ND" ** STA 18+61"ND" - STA 20+02"ND" RT STA 1+56"Z" - STA 3+00"Z"

LEGEND

EXISTING HMA PAVEMENT

EXISTING BASE AGGREGATE DENSE 3/4-INCH

EXISTING BASE AGGREGATE DENSE 1 1/4-INCH

EXISTING PCC PAVEMENT

PROPOSED PCC PAVEMENT

PROPOSED BASE AGGREGATE DENSE 3/4-INCH

PROJECT NO:1009-44-66

■W■W■ 6" TOPSOIL, SEED, FERTILIZE AND MULCH

PROPOSED HMA PAVEMENT ASPHALTIC SURFACE

HWY: VARIOUS

COUNTY: VARIOUS

TYPICAL SECTION - STH 21 & IH 39 RAMP - INTERSECTION 1

SHEET

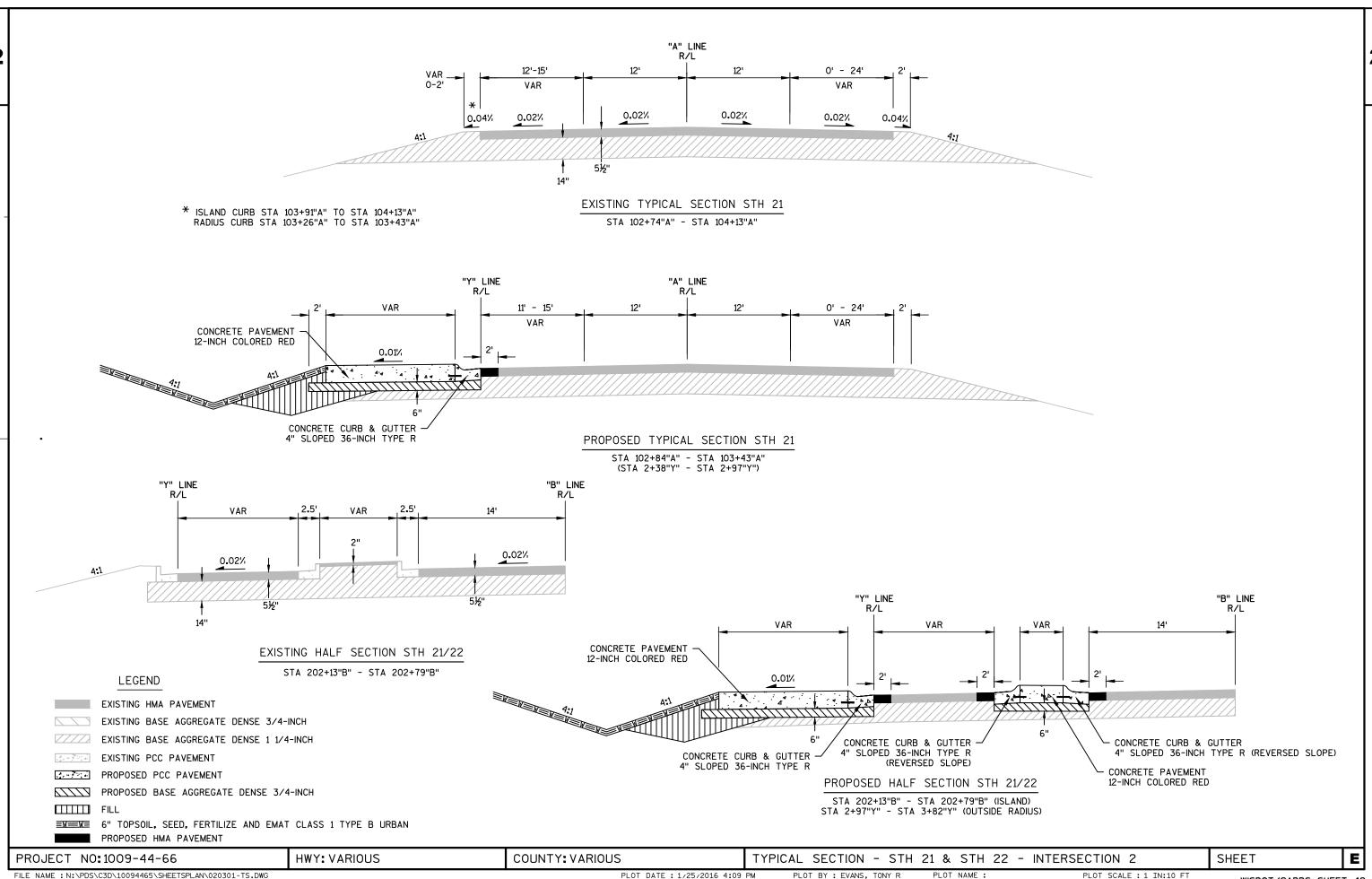
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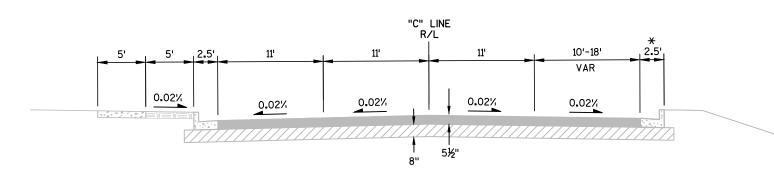
PLOT BY : EVANS, TONY R

PLOT SCALE : 1 IN:10 FT

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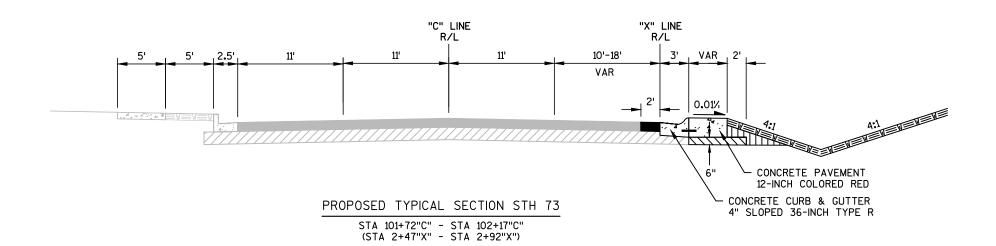
Ε



EXISTING TYPICAL SECTION STH 73

STA 101+72"C" - STA 103+05"C"

* NO CURB STA 102+17"C" TO STA 102+77"C"



LEGEND

EXISTING HMA PAVEMENT

EXISTING BASE AGGREGATE DENSE 3/4-INCH

EXISTING BASE AGGREGATE DENSE 1 1/4-INCH

EXISTING PCC PAVEMENT

PROPOSED PCC PAVEMENT

PROPOSED BASE AGGREGATE DENSE 3/4-INCH

TTTTT FILL

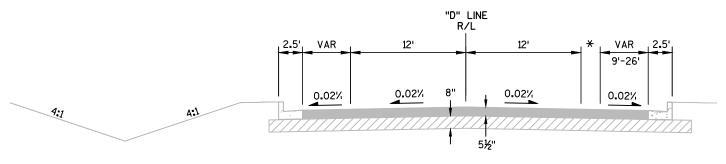
■ 6" TOPSOIL, SEED, FERTILIZE AND EMAT CLASS 1 TYPE B URBAN

PROPOSED HMA PAVEMENT

PROJECT NO:1009-44-66 HWY: VARIOUS COUNTY: VARIOUS TYPICAL SECTION - STH 21, STH 22 & STH 73 - INTERSECTION 3 SHEET

FILE NAME : N:\PDS\C3D\10094465\SHEETSPLAN\020301-TS.DWG PLOT BY: EVANS, TONY R PLOT NAME: PLOT SCALE: 1 IN:10 FT WISDOT/CADDS SHEET 42

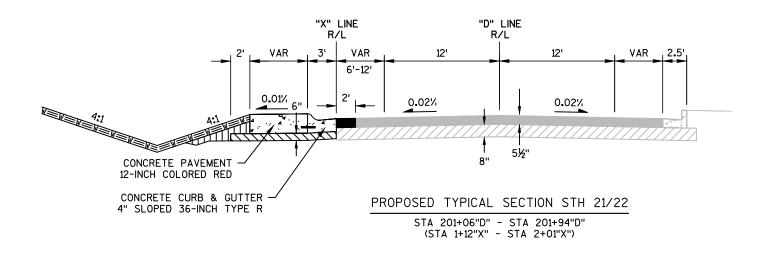


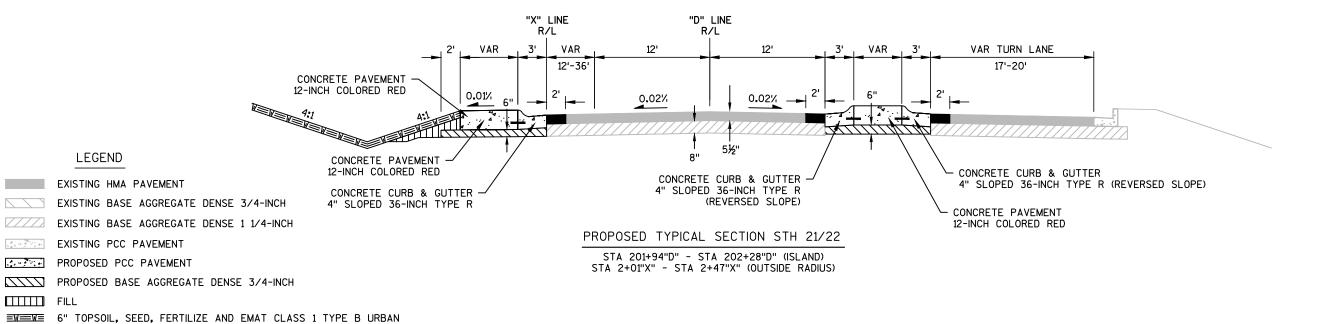


* SPLITTER ISLAND STA 201+94"D" TO STA 202+28"D" 0'-26' WIDE

EXISTING TYPICAL SECTION STH 21/22

STA 201+06"D" - STA 202+28"D"





PROJECT NO:1009-44-66

LEGEND

COUNTY: VARIOUS

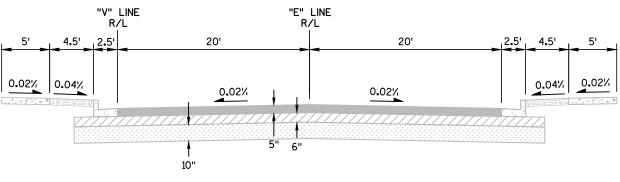
TYPICAL SECTION - STH 21, STH 22 & STH 73 - INTERSECTION 3

SHEET

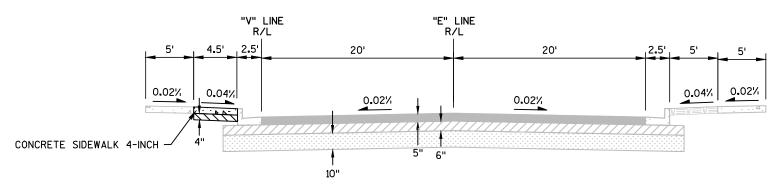
PLOT DATE: 1/25/2016 4:09 PM PLOT BY : EVANS, TONY R PLOT NAME : PLOT SCALE : 1 IN:10 FT Ε

PROPOSED HMA PAVEMENT

HWY: VARIOUS

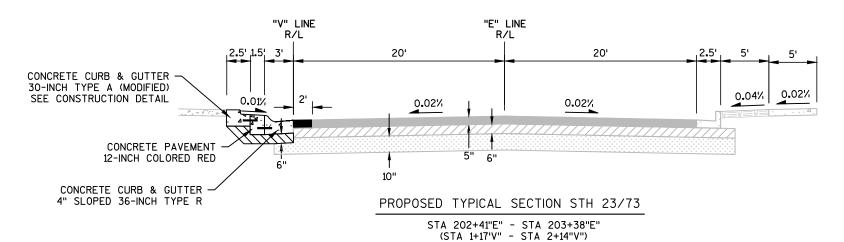


EXISTING TYPICAL SECTION STH 23/73 STA 201+88"E" - STA 203+38"E"



PROPSED TYPICAL SECTION STH 23/73

STA 202+16"E" - STA 202+41"E" (STA 0+92'V" - STA 1+17"V")



LEGEND

EXISTING HMA PAVEMENT

EXISTING BASE AGGREGATE DENSE 3/4-INCH

EXISTING BASE AGGREGATE DENSE 1 1/4-INCH

EXISTING GRANULAR BACKFILL

PROPOSED PCC PAVEMENT

PROPOSED BASE AGGREGATE DENSE 3/4-INCH

PROPOSED HMA PAVEMENT

PROJECT NO:1009-44-66 **HWY: VARIOUS**

COUNTY: VARIOUS

TYPICAL SECTION - STH 23 & STH 73 - INTERSECTION 4

SHEET

Ε

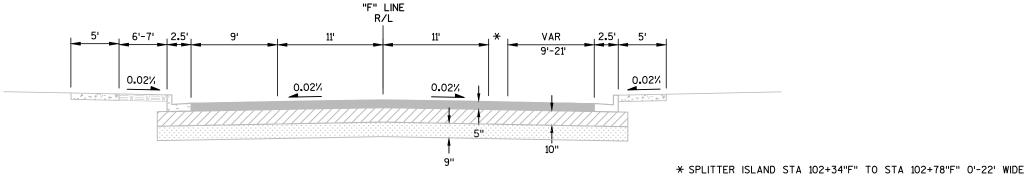
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PLOT DATE: 1/25/2016 4:09 PM

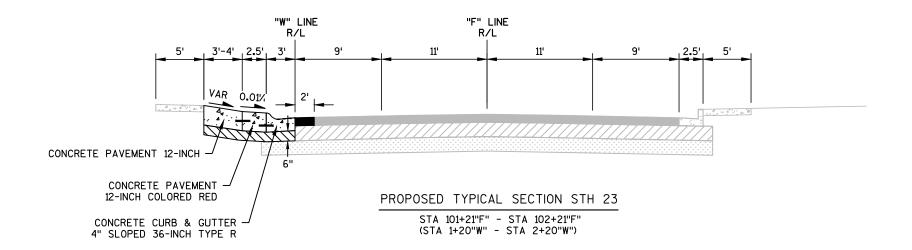
PLOT BY : EVANS, TONY R

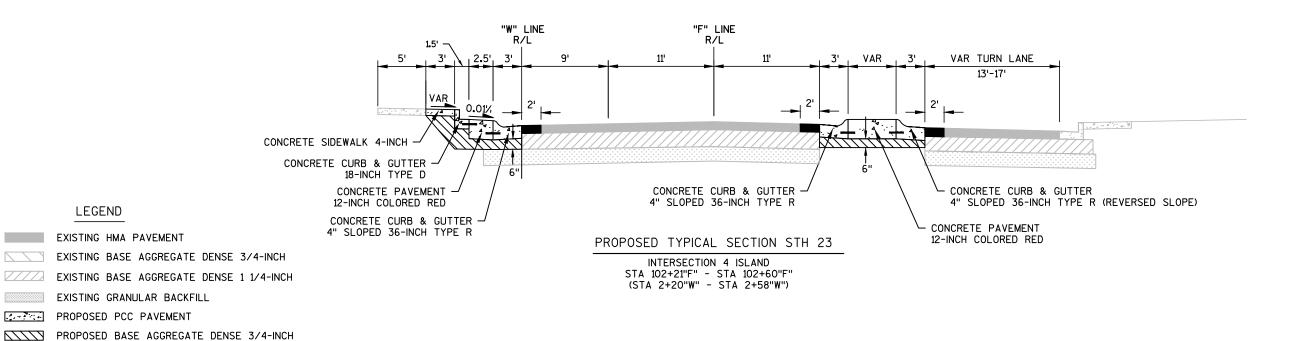
PLOT SCALE : 1 IN:10 FT





EXISTING TYPICAL SECTION STH 23 STA 101+21"F" - STA 102+78"F"





FILE NAME : N:\PDS\C3D\10094465\SHEETSPLAN\020301-TS.DWG

PROPOSED HMA PAVEMENT

HWY: VARIOUS

LEGEND

EXISTING HMA PAVEMENT

PROPOSED PCC PAVEMENT

PROJECT NO:1009-44-66

PLOT DATE: 1/25/2016 4:09 PM

COUNTY: VARIOUS

PLOT BY : EVANS, TONY R

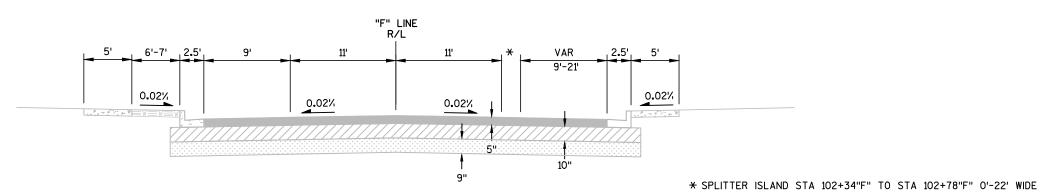
TYPICAL SECTION - STH 23 & STH 73 - INTERSECTION 4

PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 42

SHEET

E



EXISTING TYPICAL SECTION STH 23 STA 101+21"F" - STA 102+78"F"

"F" LINE "W" LINE R/L R/L VAR , 3' , VAR TURN LANE 13'-17' 0.01/ CONCRETE PAVEMENT 12-INCH CONCRETE PAVEMENT 12-INCH COLORED RED CONCRETE CURB & GUTTER -CONCRETE CURB & GUTTER 4" SLOPED 36-INCH TYPE R 4" SLOPED 36-INCH TYPE R (REVERSED SLOPE) CONCRETE CURB & GUTTER CONCRETE PAVEMENT 12-INCH COLORED RED 4" SLOPED 36-INCH TYPE R PROPOSED TYPICAL SECTION STH 23 INTERSECTION 4 ISLAND STA 102+60"F" - STA 102+78"F" (STA 2+58"W" - STA 2+71"W")

LEGEND

EXISTING HMA PAVEMENT

EXISTING BASE AGGREGATE DENSE 3/4-INCH

EXISTING BASE AGGREGATE DENSE 1 1/4-INCH

EXISTING GRANULAR BACKFILL

PROPOSED PCC PAVEMENT

PROPOSED BASE AGGREGATE DENSE 3/4-INCH

PROPOSED HMA PAVEMENT

HWY: VARIOUS

COUNTY: VARIOUS

TYPICAL SECTION - STH 23 & STH 73 - INTERSECTION 4

SHEET

FILE NAME : N:\PDS\C3D\10094465\SHEETSPLAN\020301-TS.DWG

PROJECT NO:1009-44-66

PLOT DATE: 1/25/2016 4:09 PM

PLOT BY : EVANS, TONY R

PLOT NAME :

PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 42

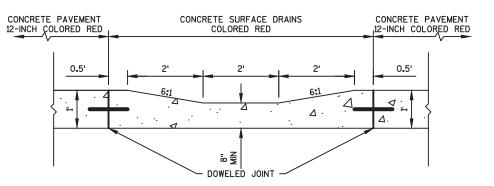
E

CURB TRANSITION DETAIL

TRANSITION FOR CONCRETE CURB & GUTTER 30-INCH TYPE A/D TO CONCRETE CURB & GUTTER 36-INCH TYPE R

TO BE MEASURED AND PAID FOR AS CONCRETE CURB & GUTTER 36-INCH TYPE R

HWY: VARIOUS



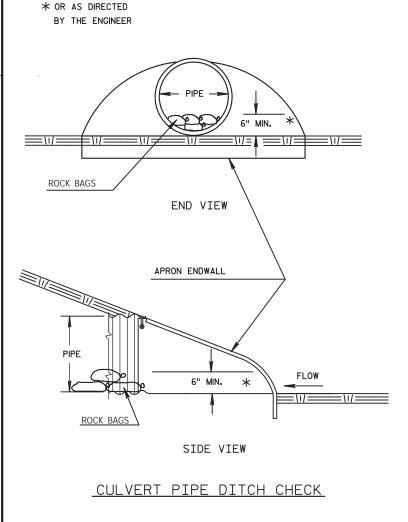
CONCRETE SURFACE DRAIN THROUGH CONCRETE PAVEMENT 12-INCH DETAIL

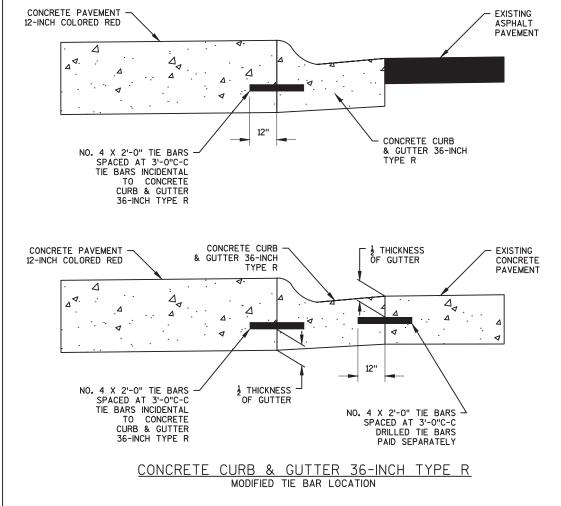
INTERSECTION 3

FLUME LONGITUDINAL SLOPE TO MATCH CONCRETE PAVEMENT 12-INCH SLOPE. CONCRETE SURFACE DRAIN TO BE COLORED RED

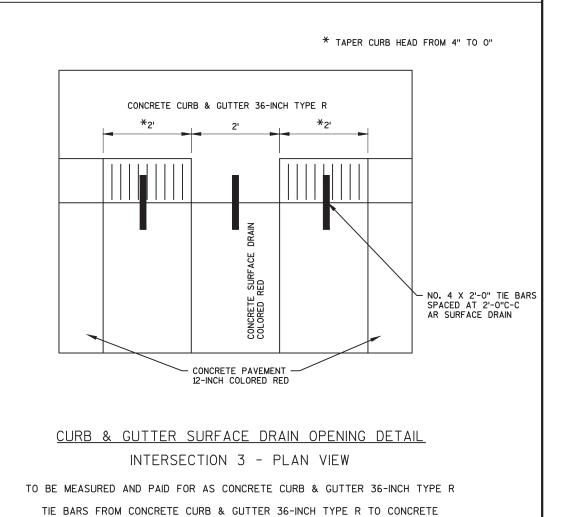
TO BE MEASURED AND PAID FOR AS CONCRETE SURFACE DRAINS COLORING CONCRETE RED WILL BE PAID FOR SEPARATELY

DOWEL BARS INCIDENTAL TO CONCRETE PAVEMENT 12-INCH





COUNTY: VARIOUS



PROJECT NO:1009-44-66

PLOT DATE : 2/25/2016 1:31 PM

PLOT BY : EVANS, TONY R

CONSTRUCTION DETAILS

PLOT SCALE : 1 IN:10 FT

SURFACE DRAIN ARE INCIDENTAL TO CONCRETE CURB & GUTTER 36-INCH TYPE R

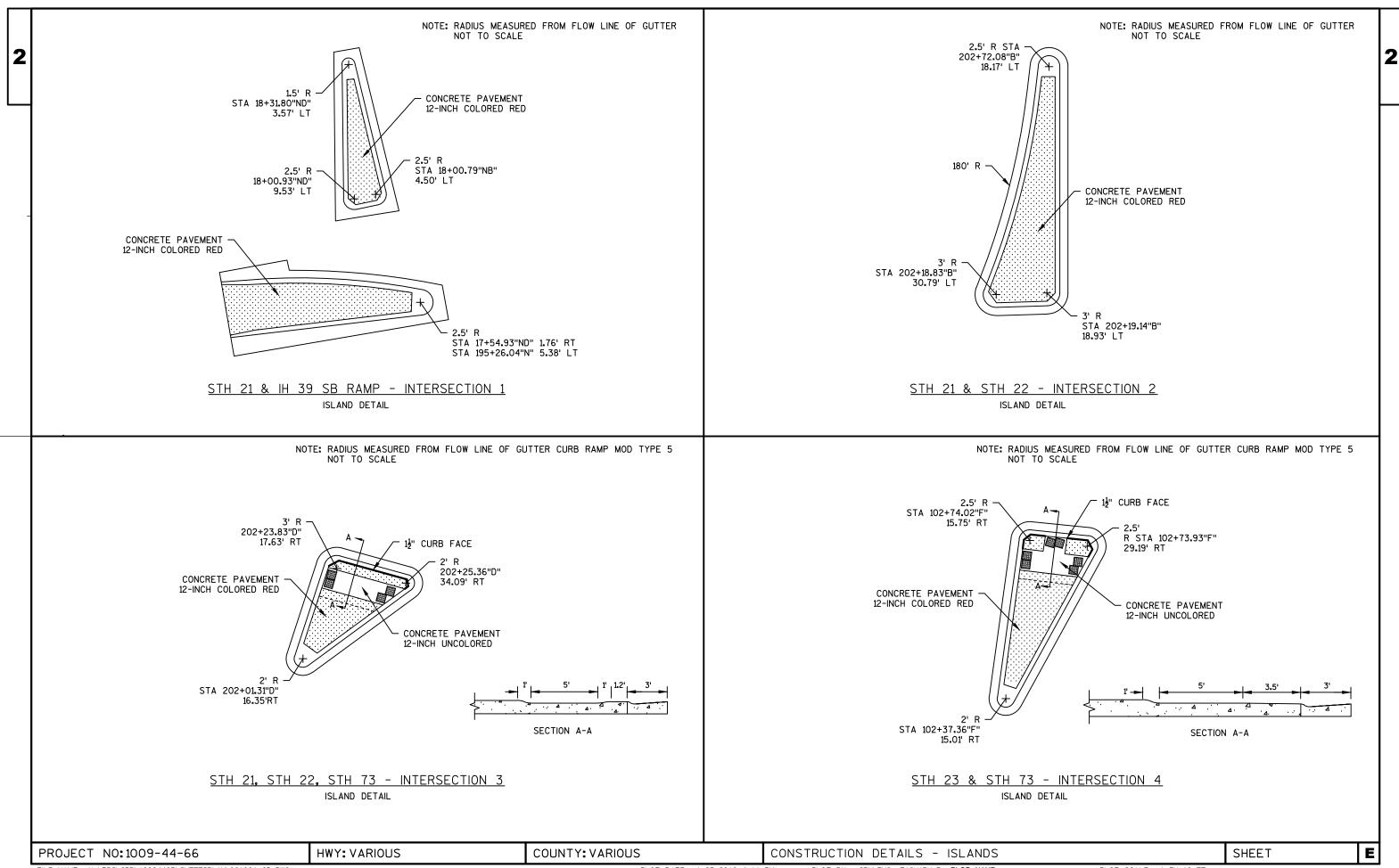
BID ITEM

WISDOT/CADDS SHEET 42

FILE NAME : N:\PDS\C3D\10094465\SHEETSPLAN\021001-CD.DWG

PLOT NAME :

SHEET



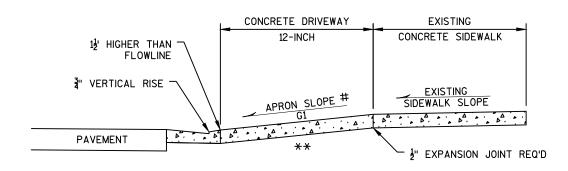
INTERSECTION 4

* VARY SLOPE TO MATCH INTO EXISTING SIDEWALK

** 1% NORMAL REJECT SLOPE

*** TRANSITION CURB FACE FROM 4" TO 0" WITHIN 6 FEET TO MATCH DRIVEWAY AND EXISTING GROUND TIEBAR AND ½" EXPANSION JOINT INCIDENTAL TO CURB AND GUTTER 30-INCH TYPE A MODIFIED

DRIVEWAY ENTRANCE WITH SIDEWALK, CURB & GUTTER

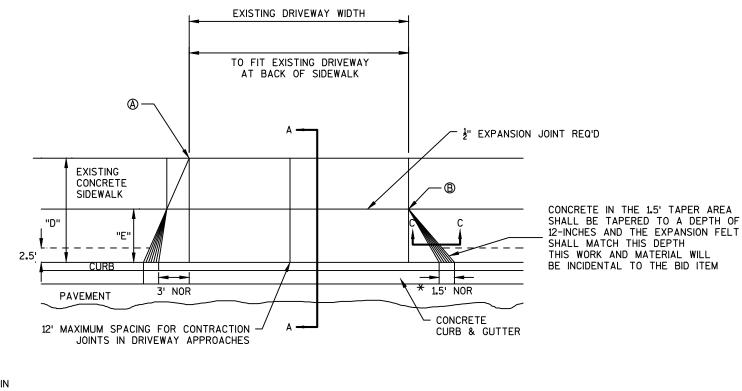


SECTION A-A

6" BASE AGGREGATE DENSE ¾" REQ'D UNDER CONCRETE DRWY

TERRACE	APRON SLOPE (G1)		
WIDTH	MIN %	DESIRABLE %	MAX %
3 FT	7.0	8.5	9.0
4 FT	5.0	7.0	9.0
5 FT	4.0	7.0	9.0
6 FT	4.0	7.0	9.0
7 FT	3.5	7.0	9.0
8 FT	3.0	7.0	9.0

NOTE: ALGEBRAIC DIFFERENCE BETWEEN TANGENT GRADES G1 & G2 TO NOT EXCEED 15%



STH 23 & STH 73 - INTERSECTION 4 DRIVEWAY ENTRANCE WITH SIDEWALK, CURB AND GUTTER

- WHEN "D" IS 13' OR LESS, ALIGN TAPER WITH BACK OF SIDEWALK
- WHEN "D" IS GREATER THAN 13', ALIGN TAPER WITH FRONT OF SIDEWALK
- WHEN "E" =0 MAKE CURB TAPER 5'

PROJECT NO:1009-44-66 E HWY: VARIOUS COUNTY: VARIOUS CONSTRUCTION DETAILS SHEET

FILE NAME: N:\PDS\C3D\10094465\SHEETSPLAN\021001-CD.DWG

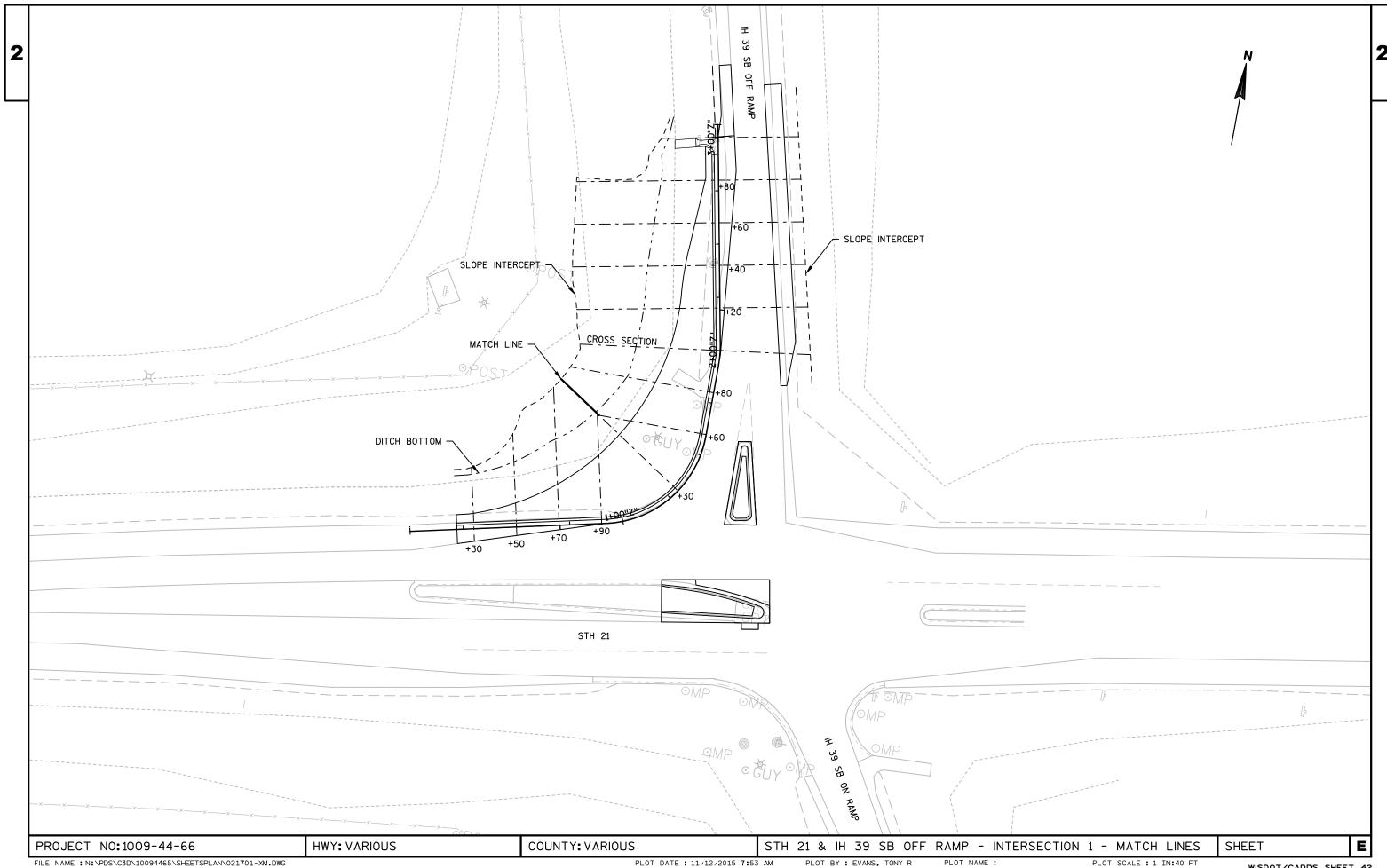
CONCRETE DRIVEWAY 12-INCH

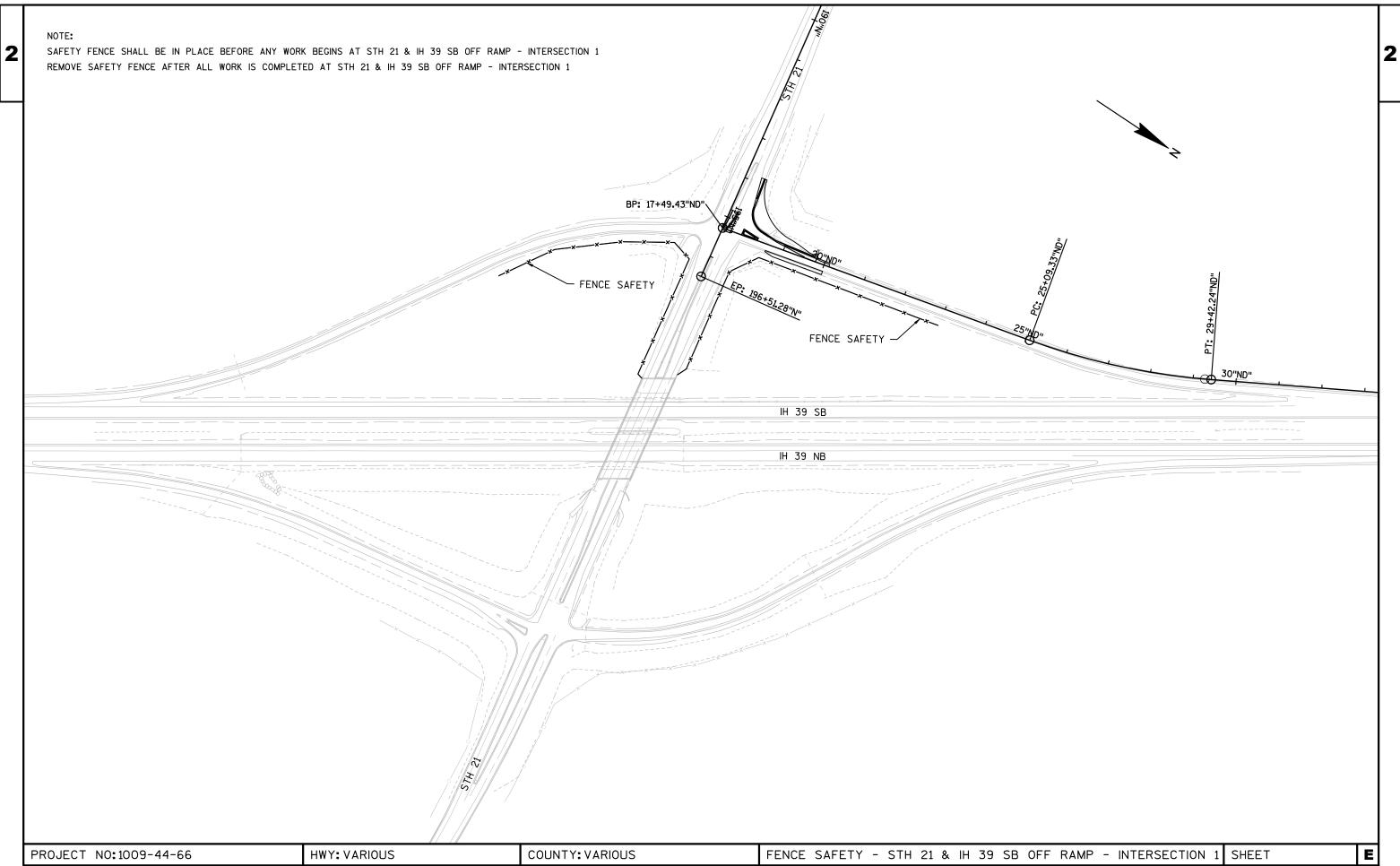
SECTION C-C

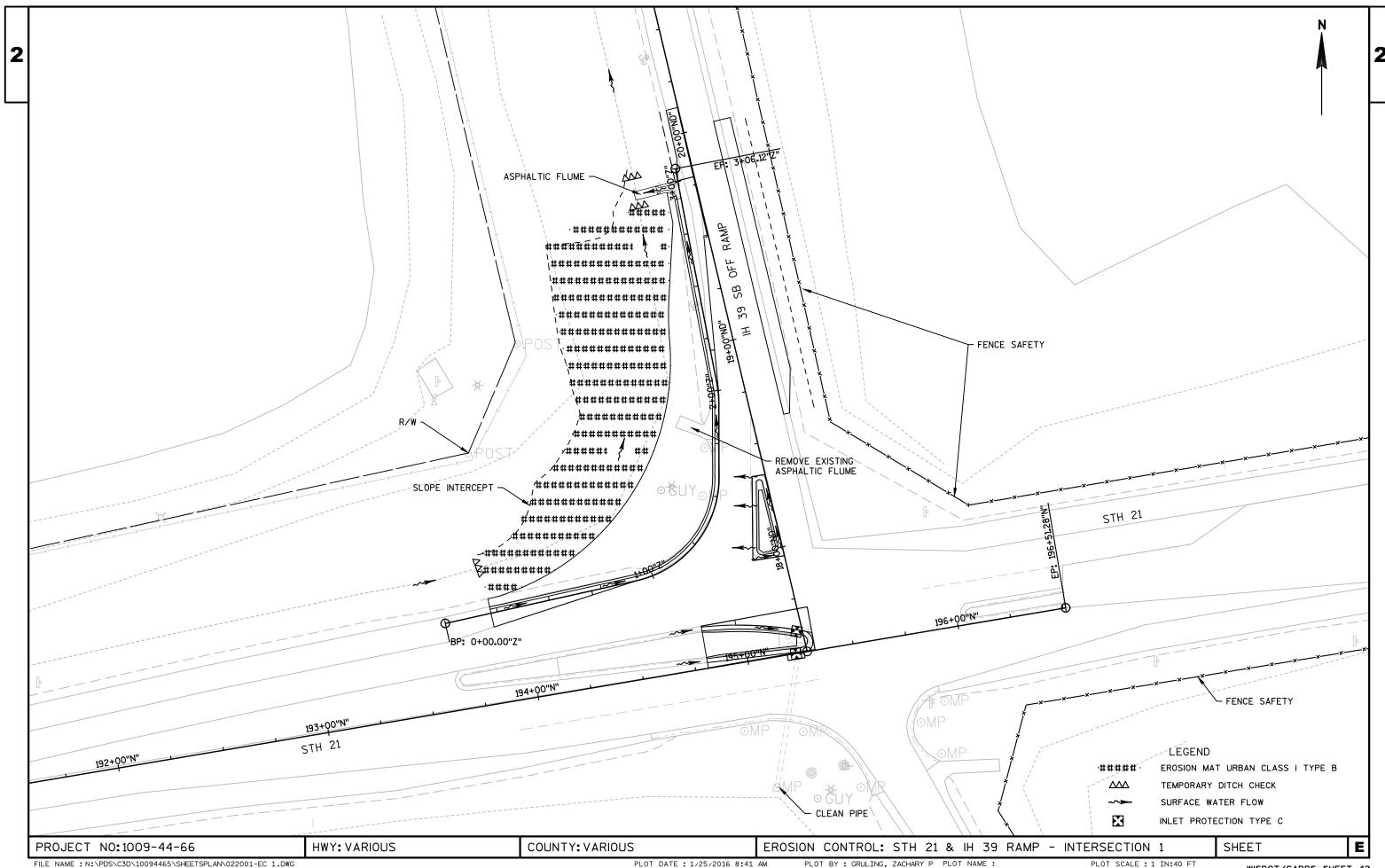
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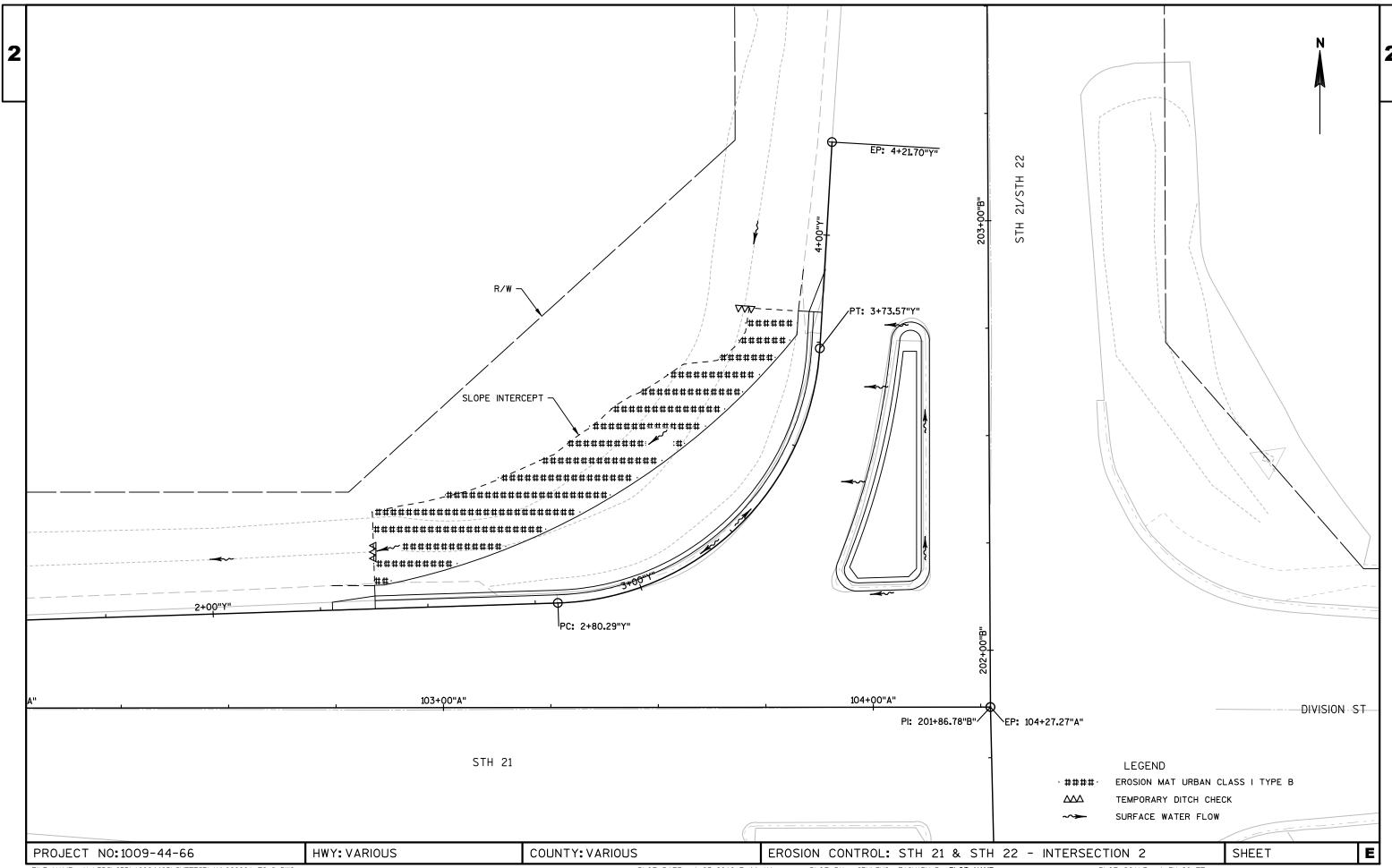
PLOT BY: GRULING, ZACHARY P PLOT NAME:

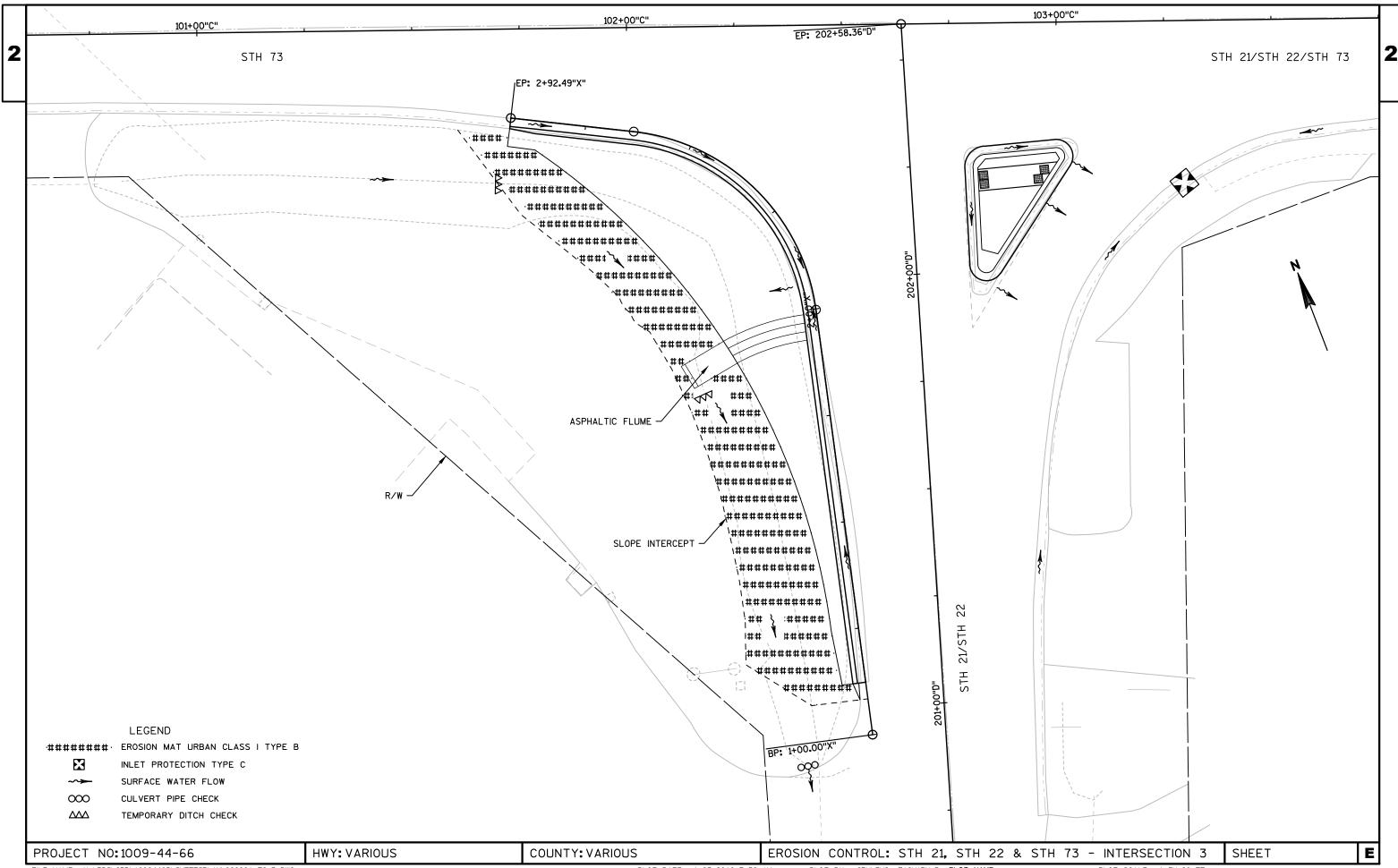
PLOT SCALE : 1 IN:10 FT

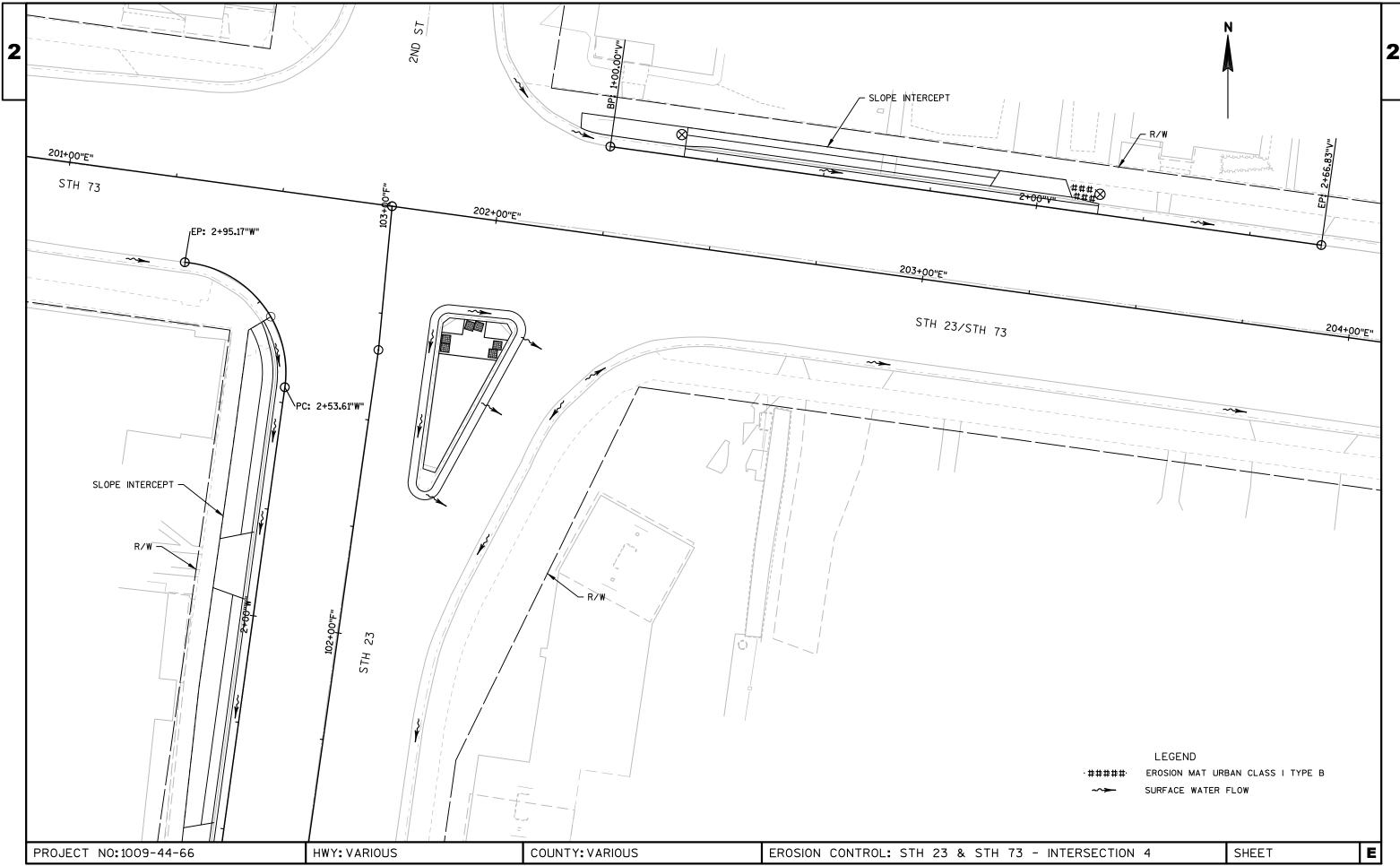


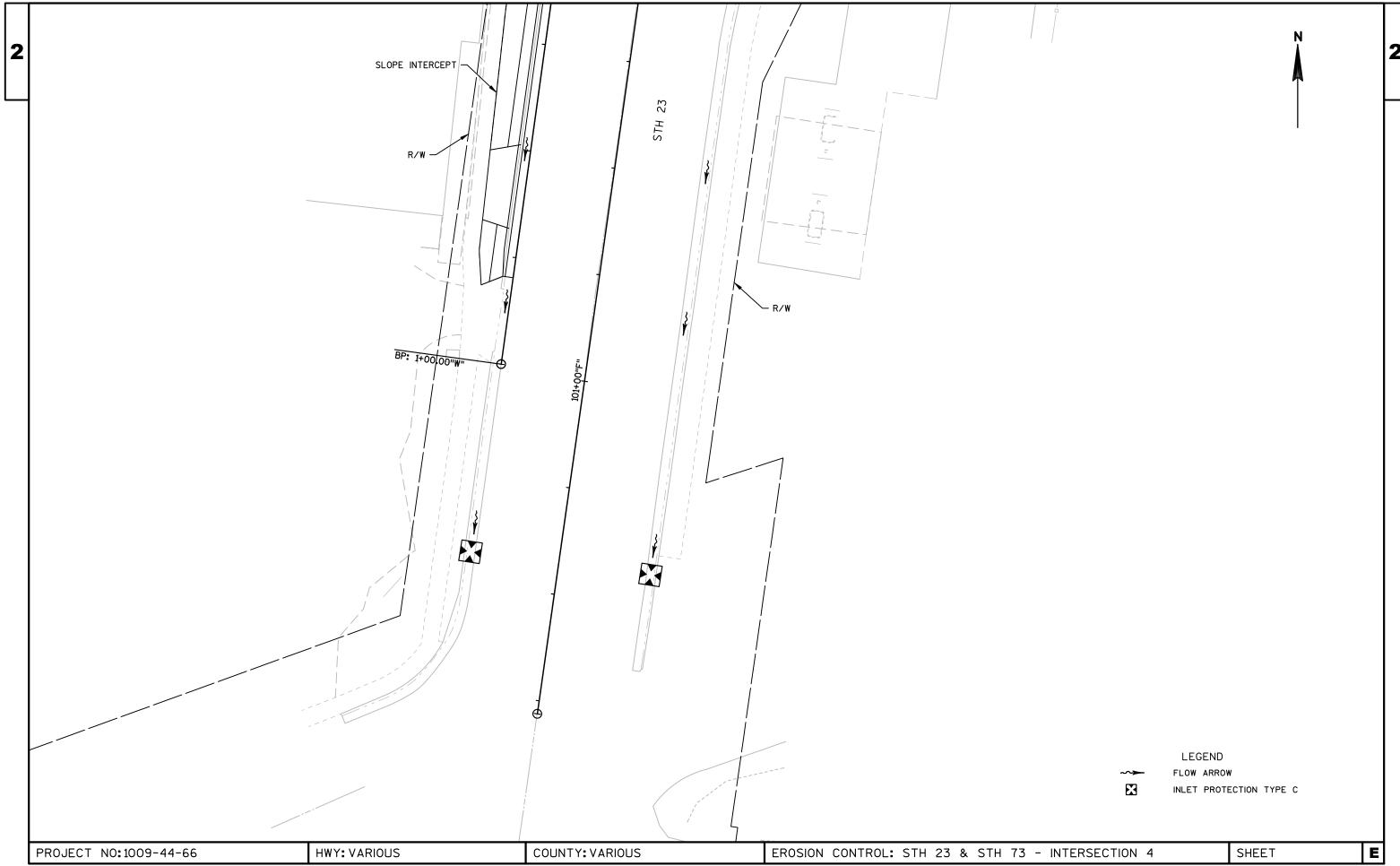


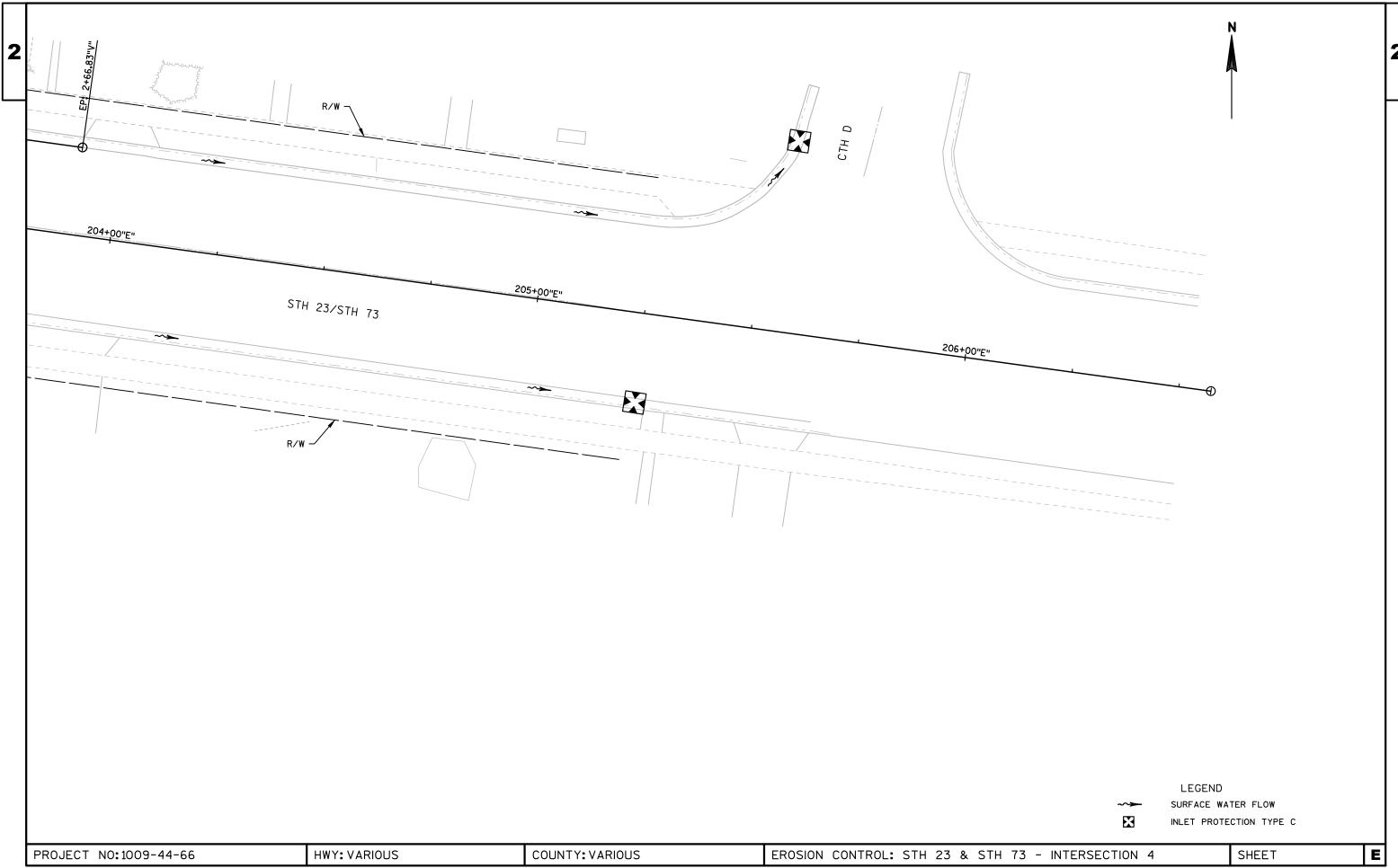


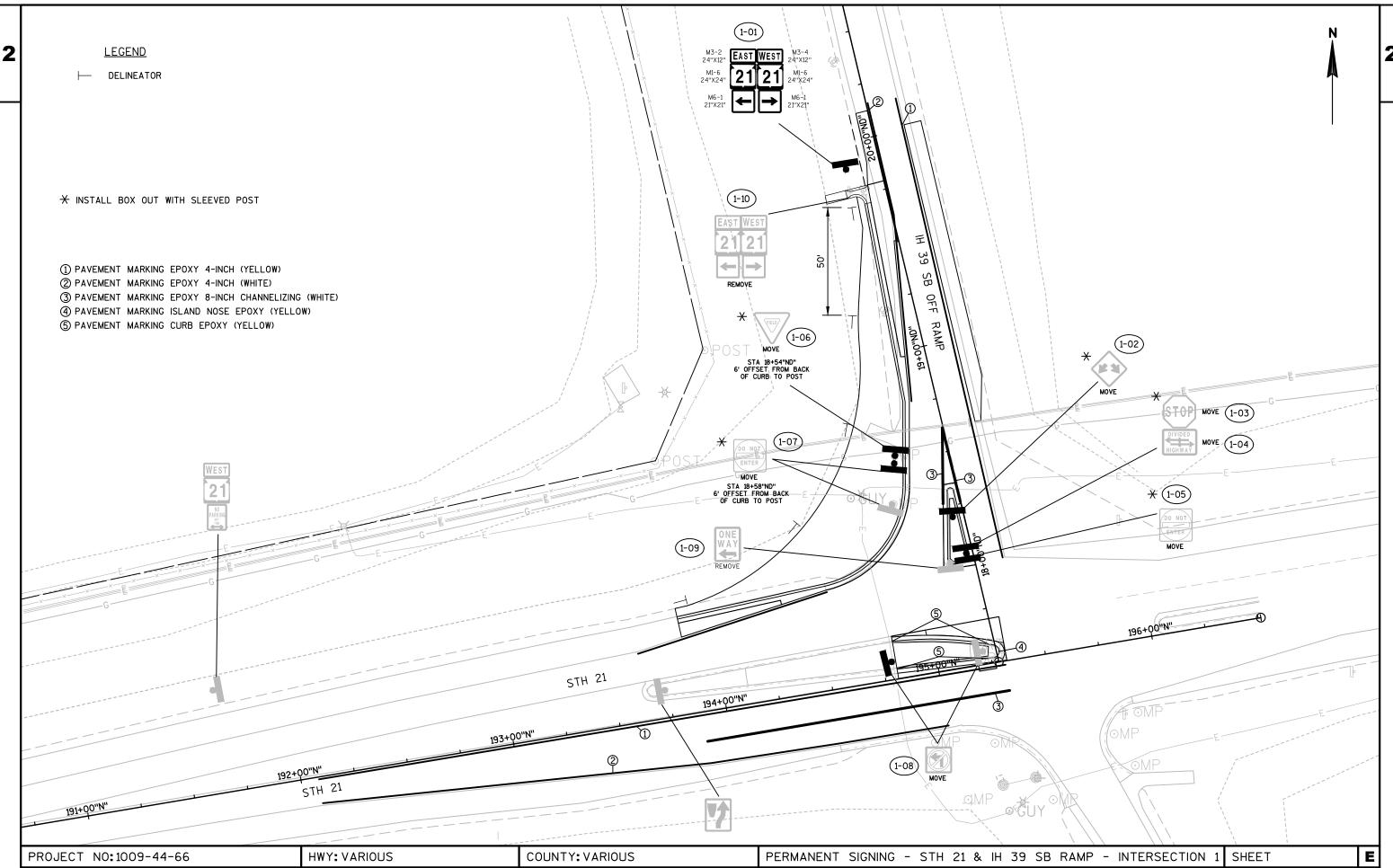


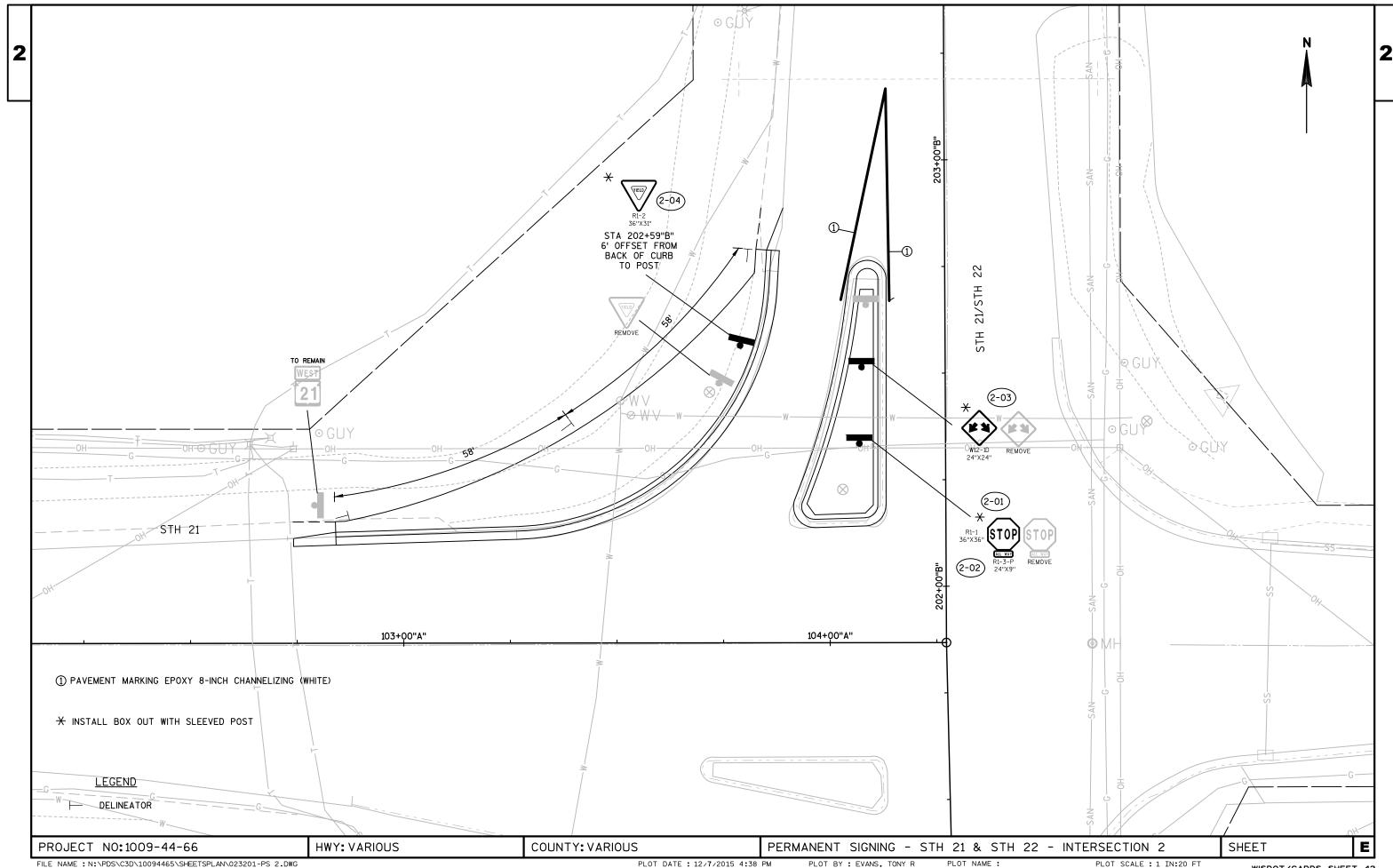


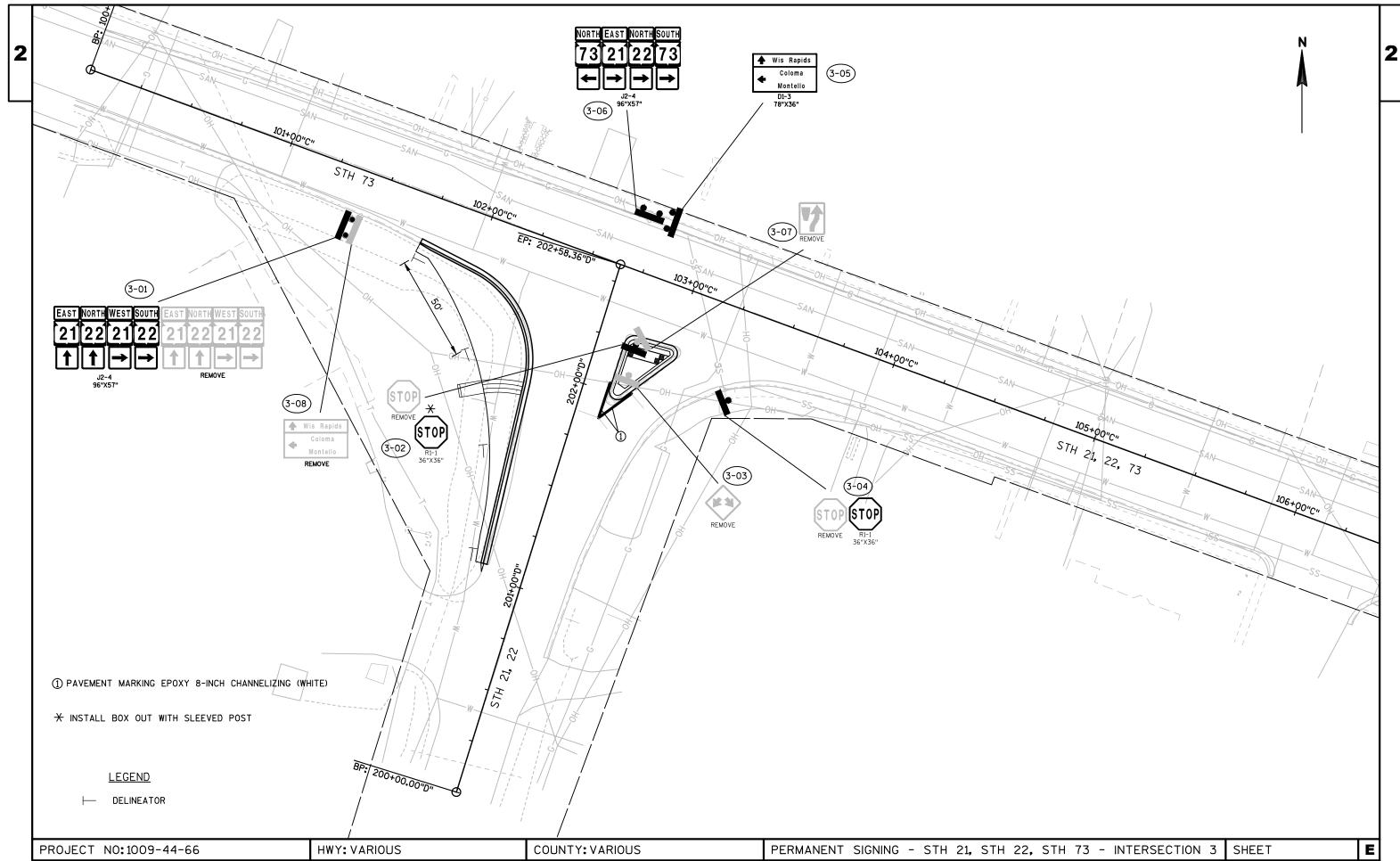


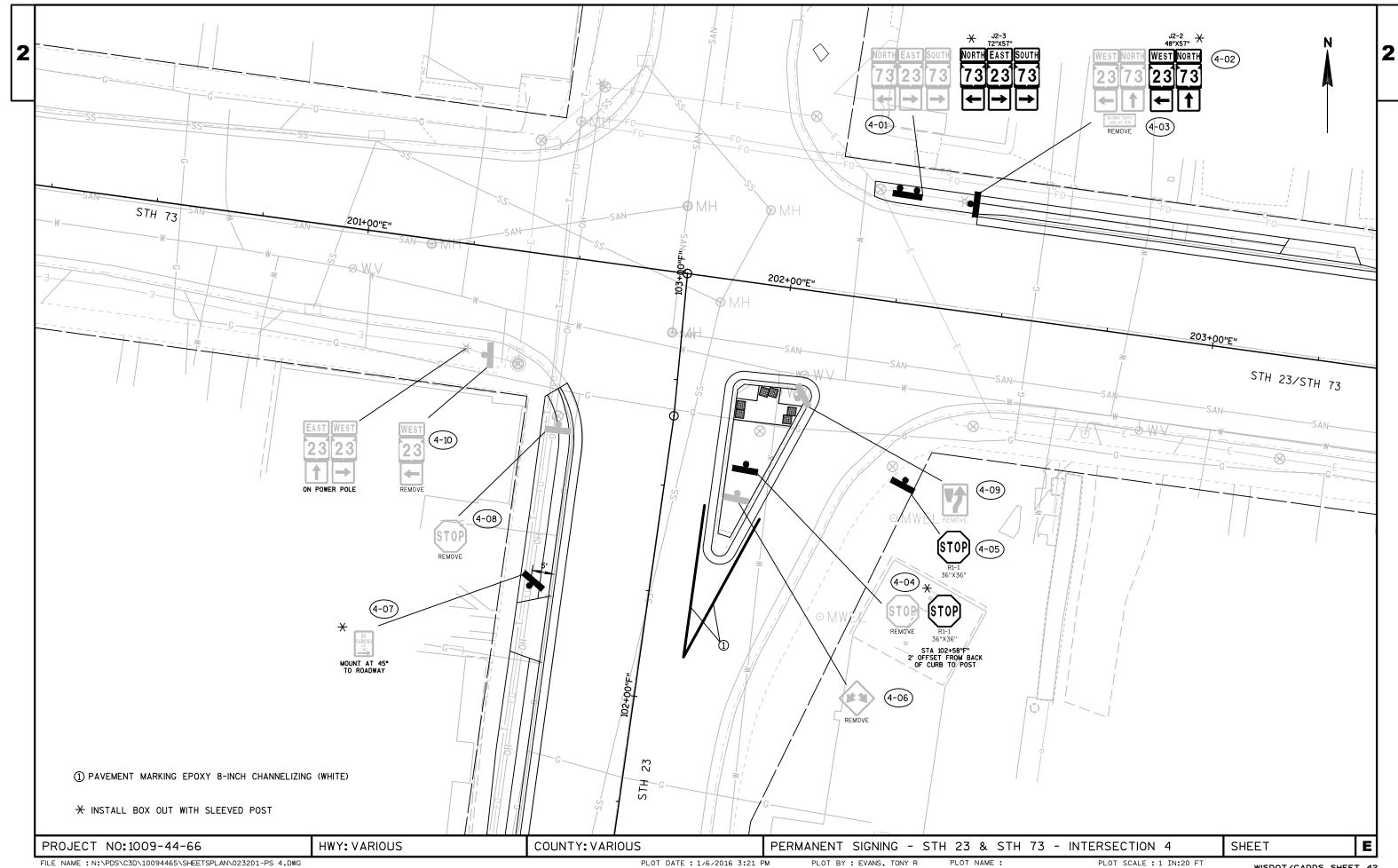


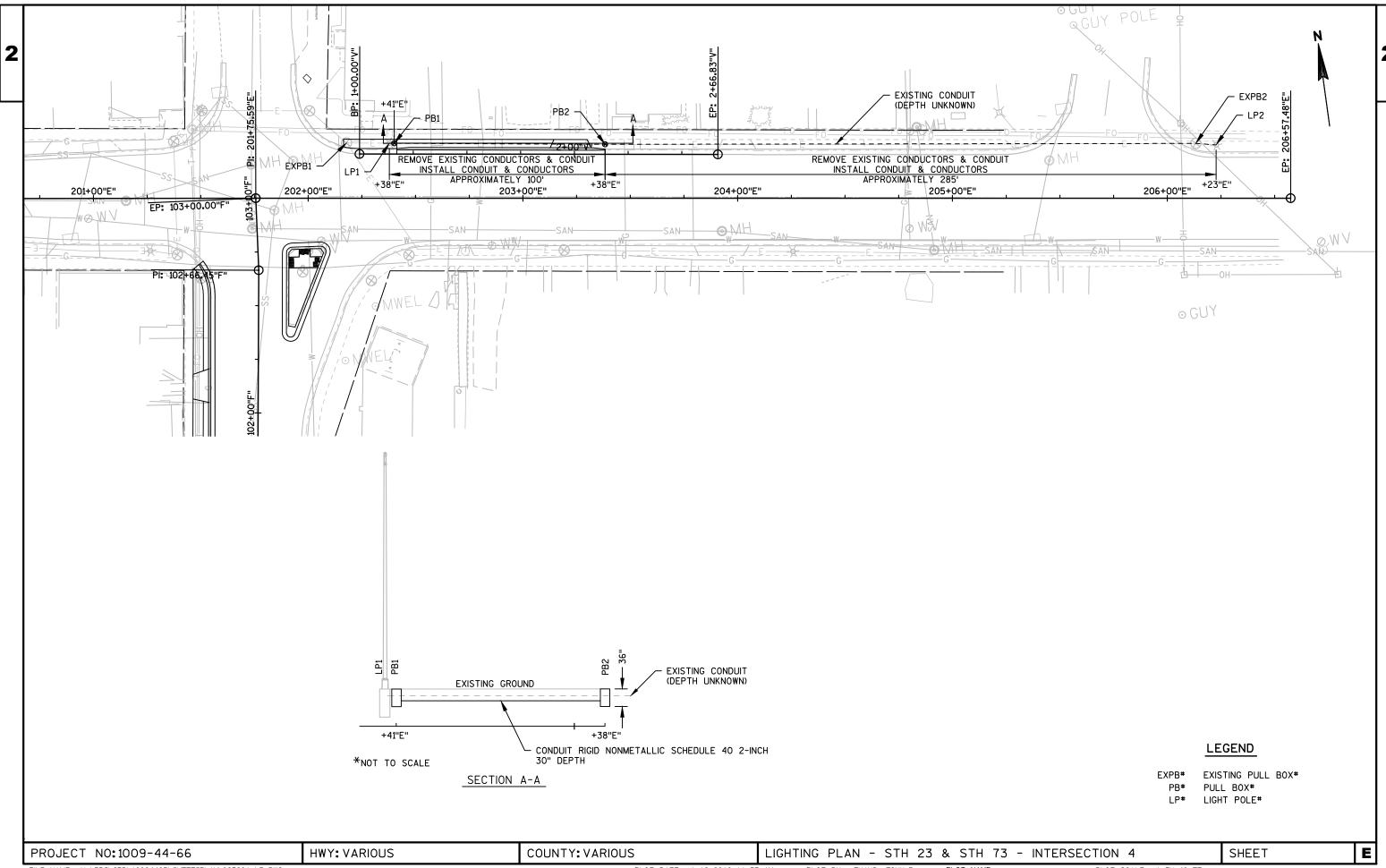


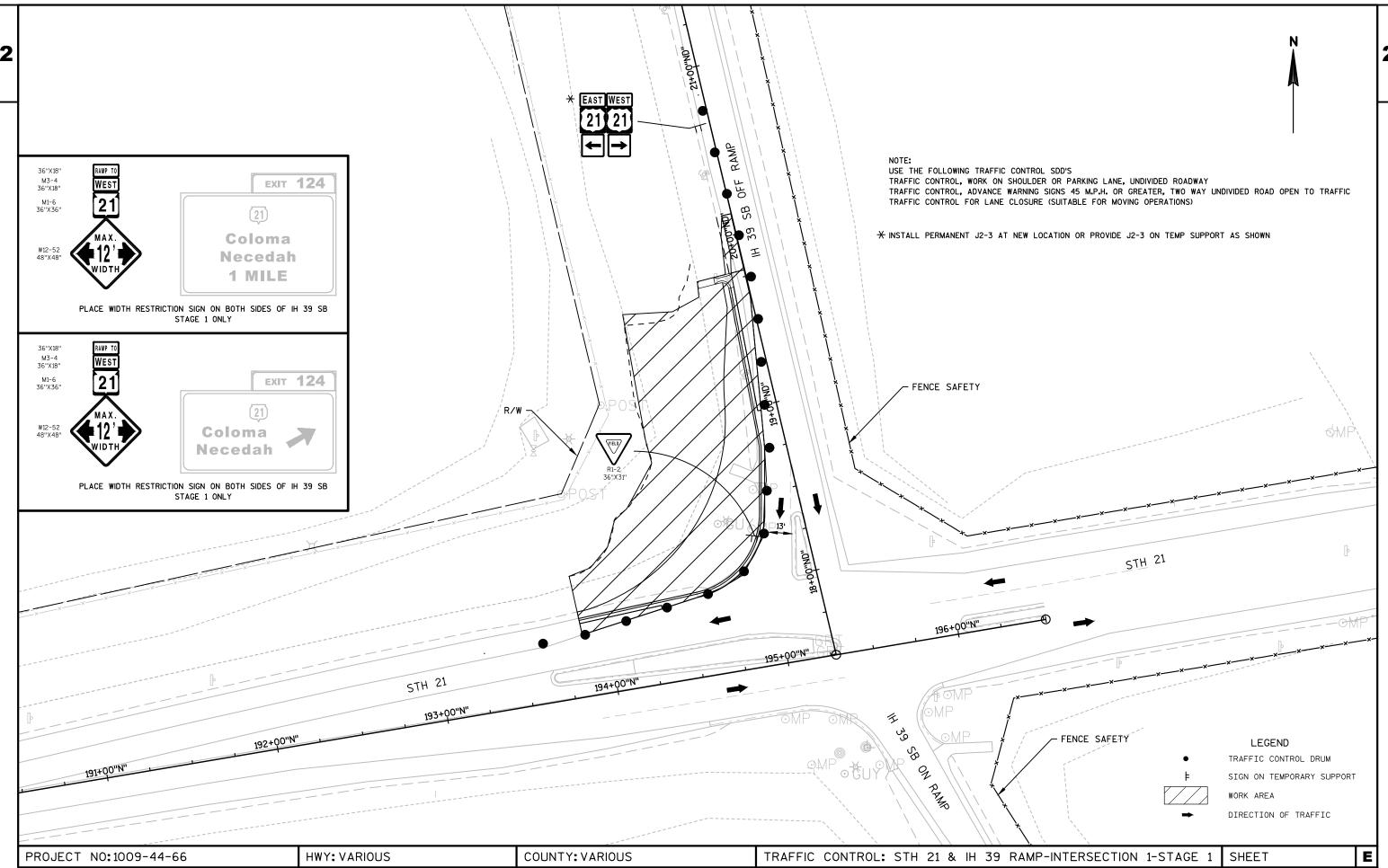


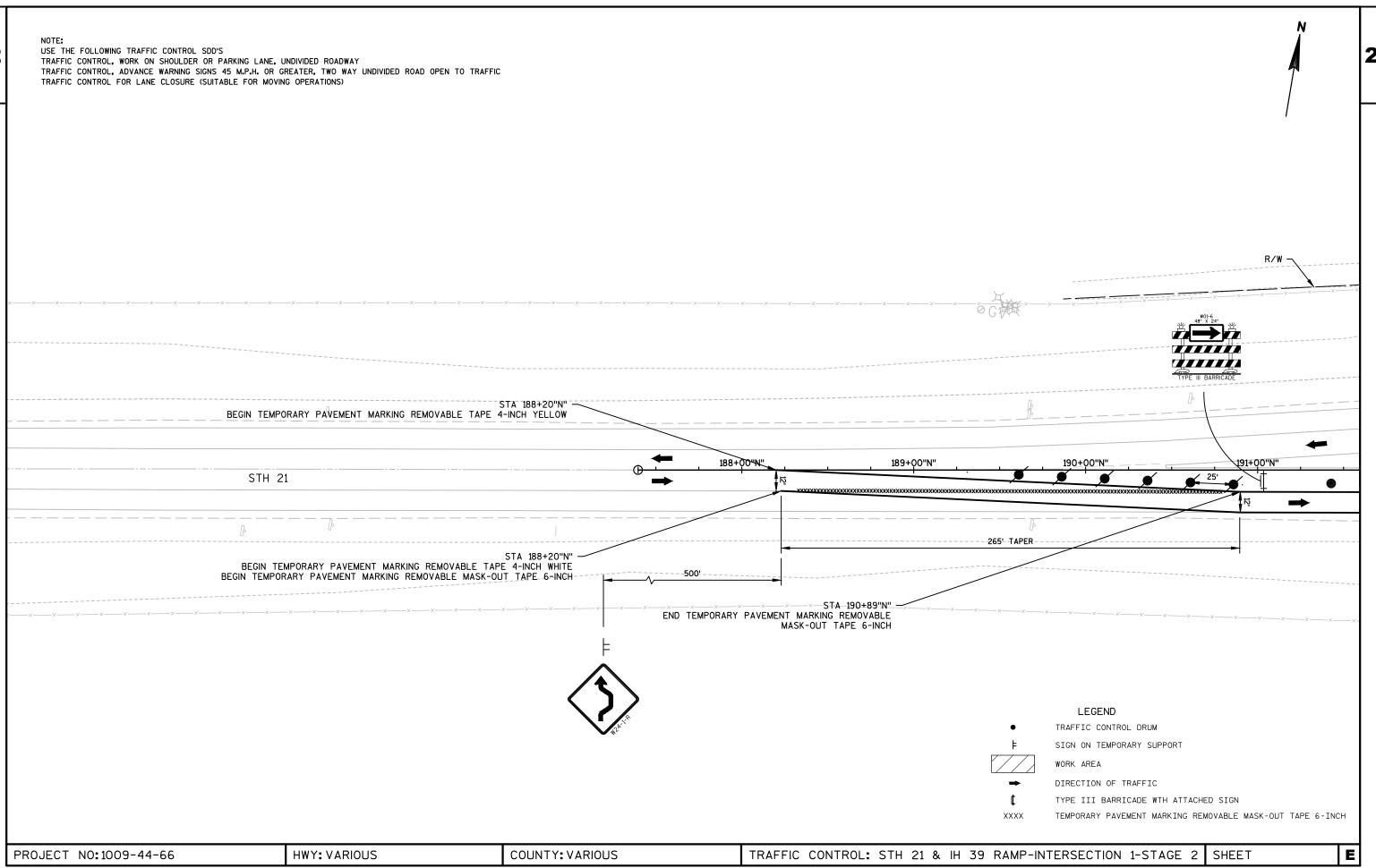


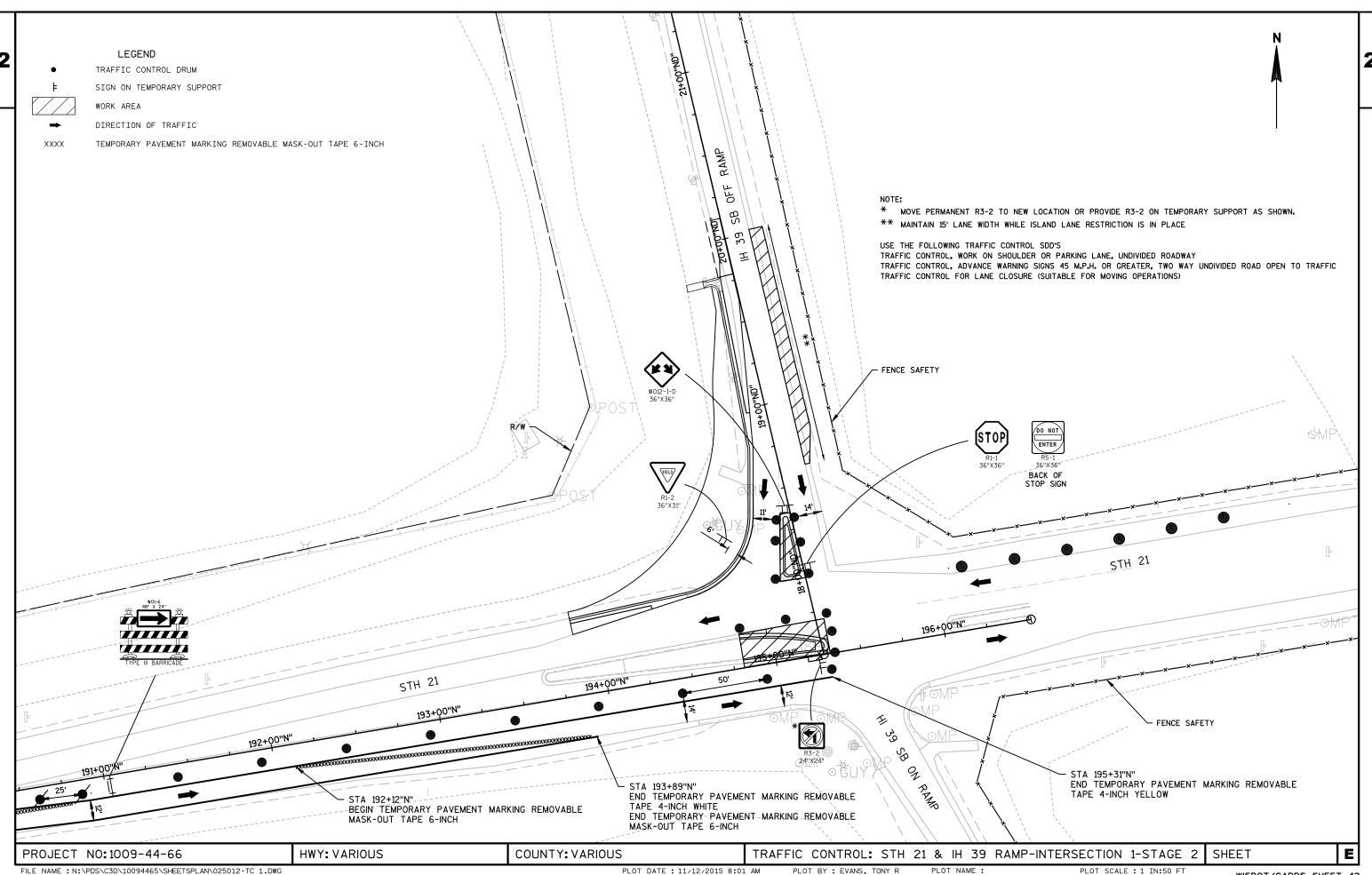


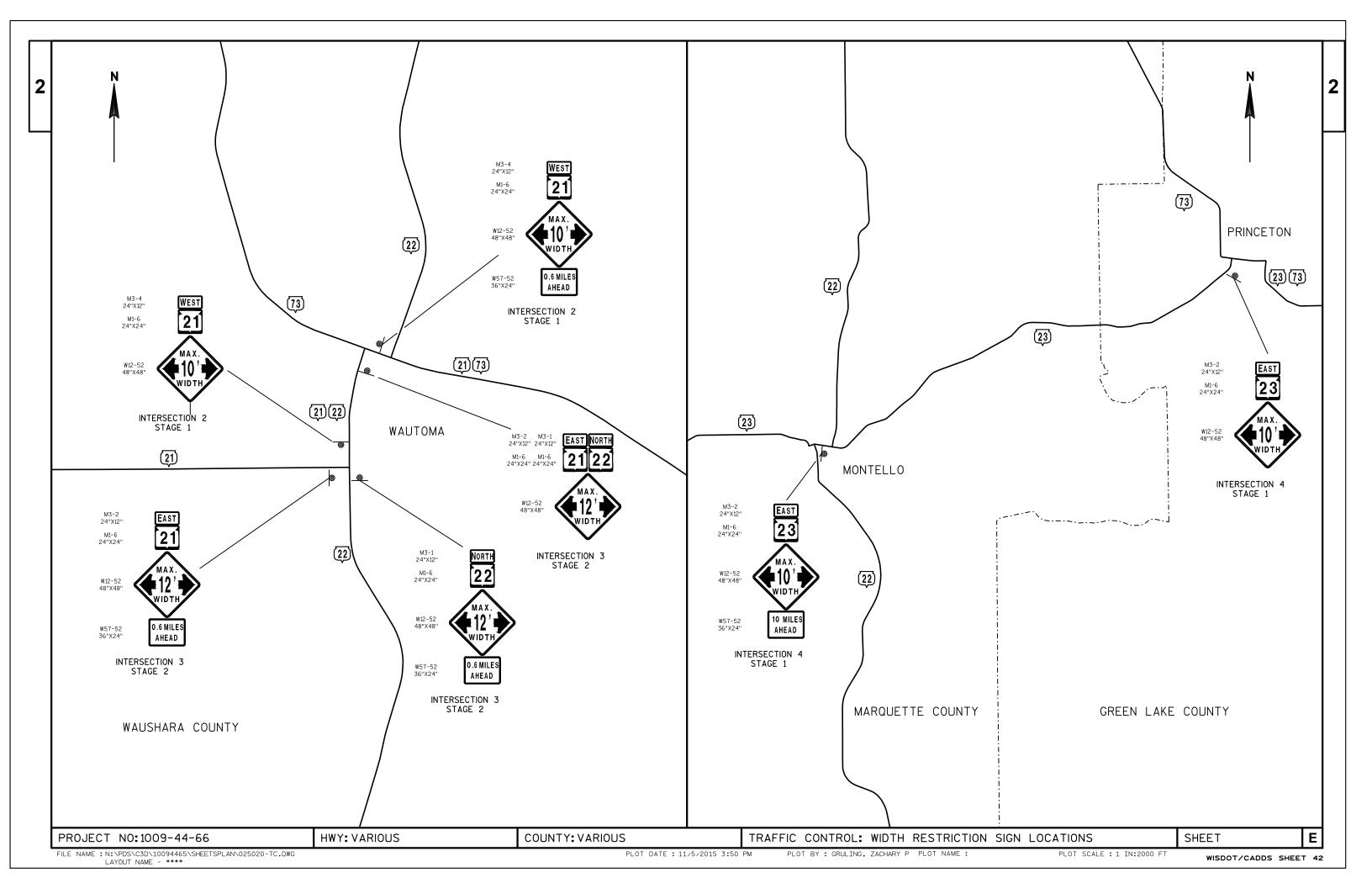


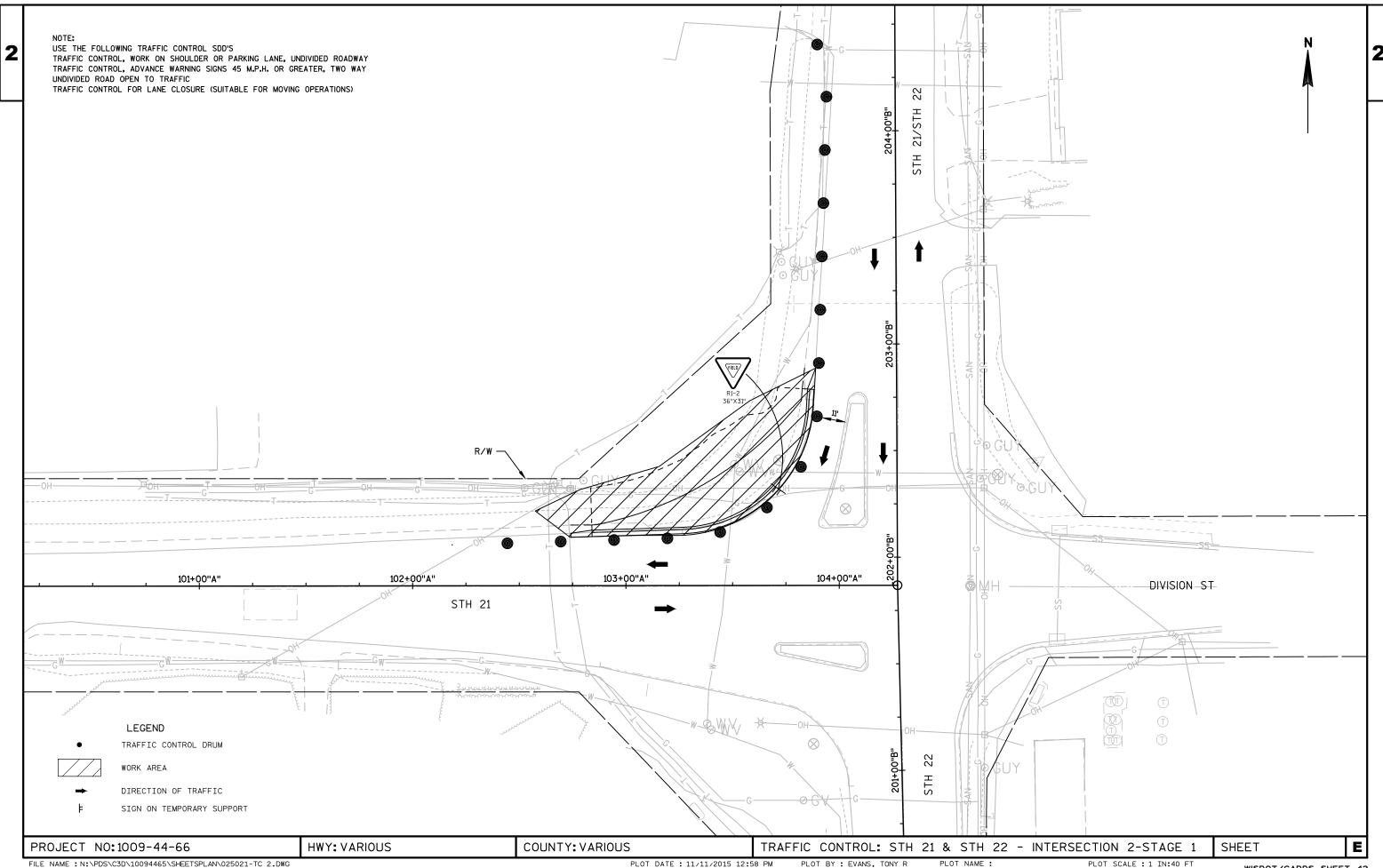


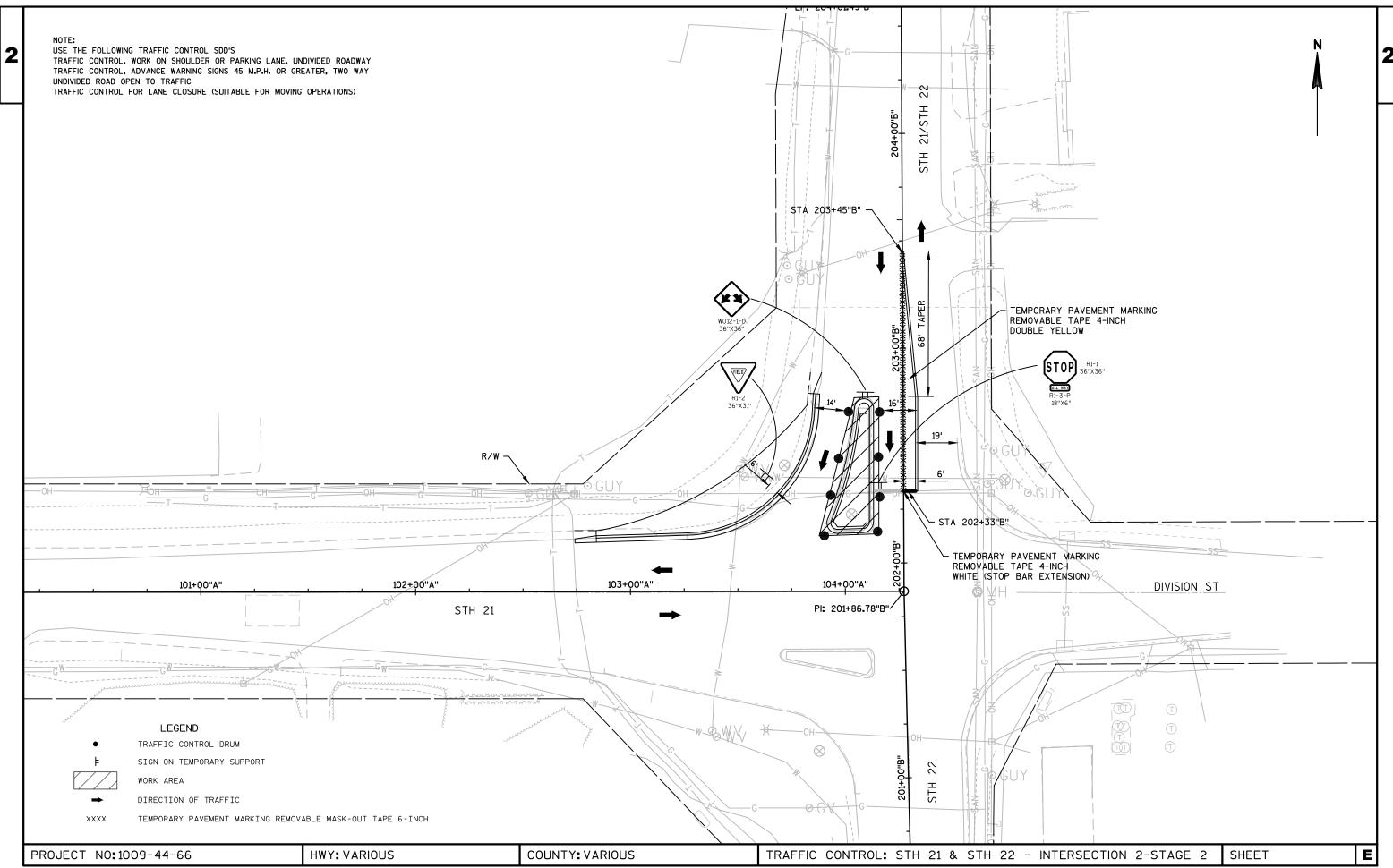


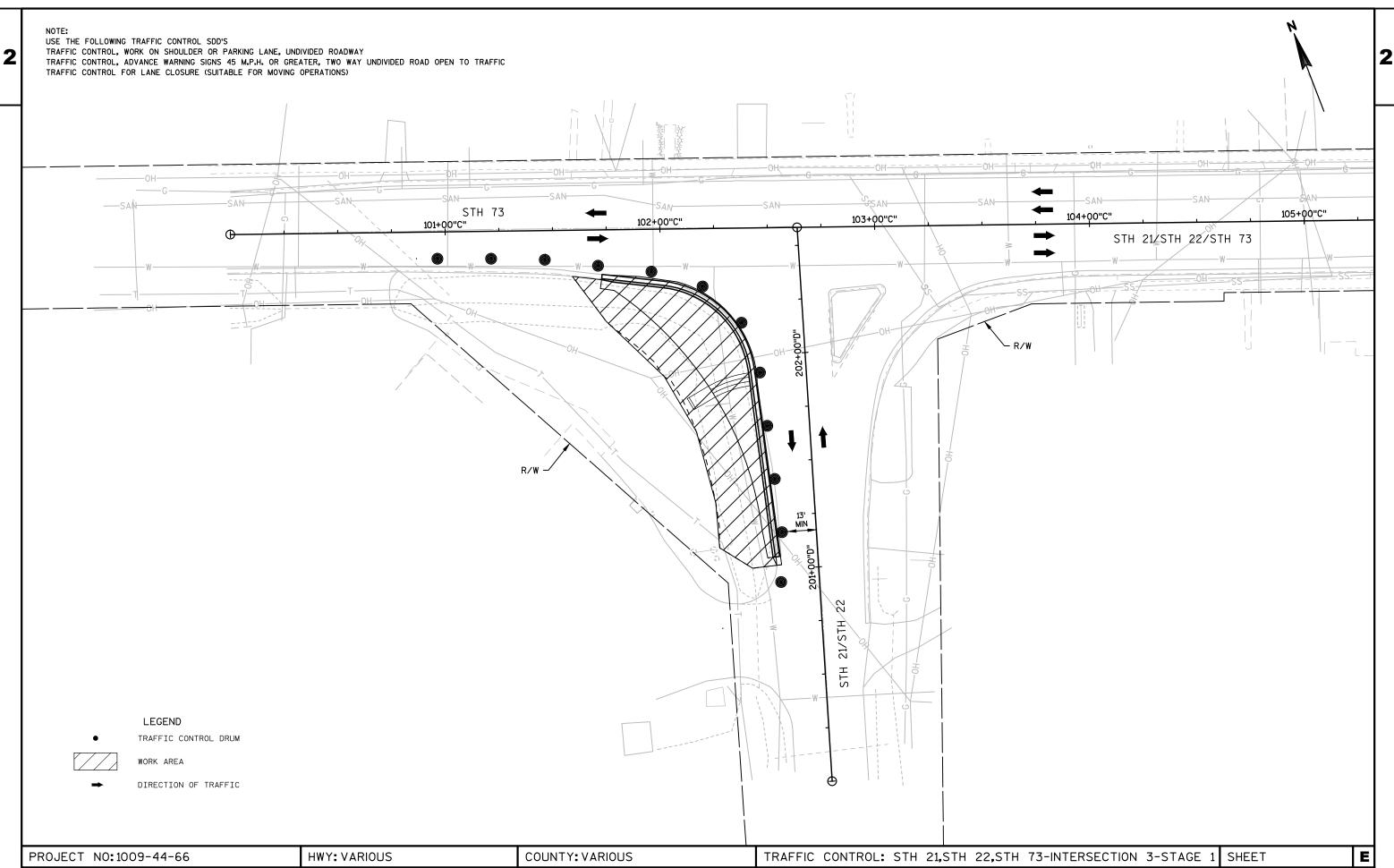


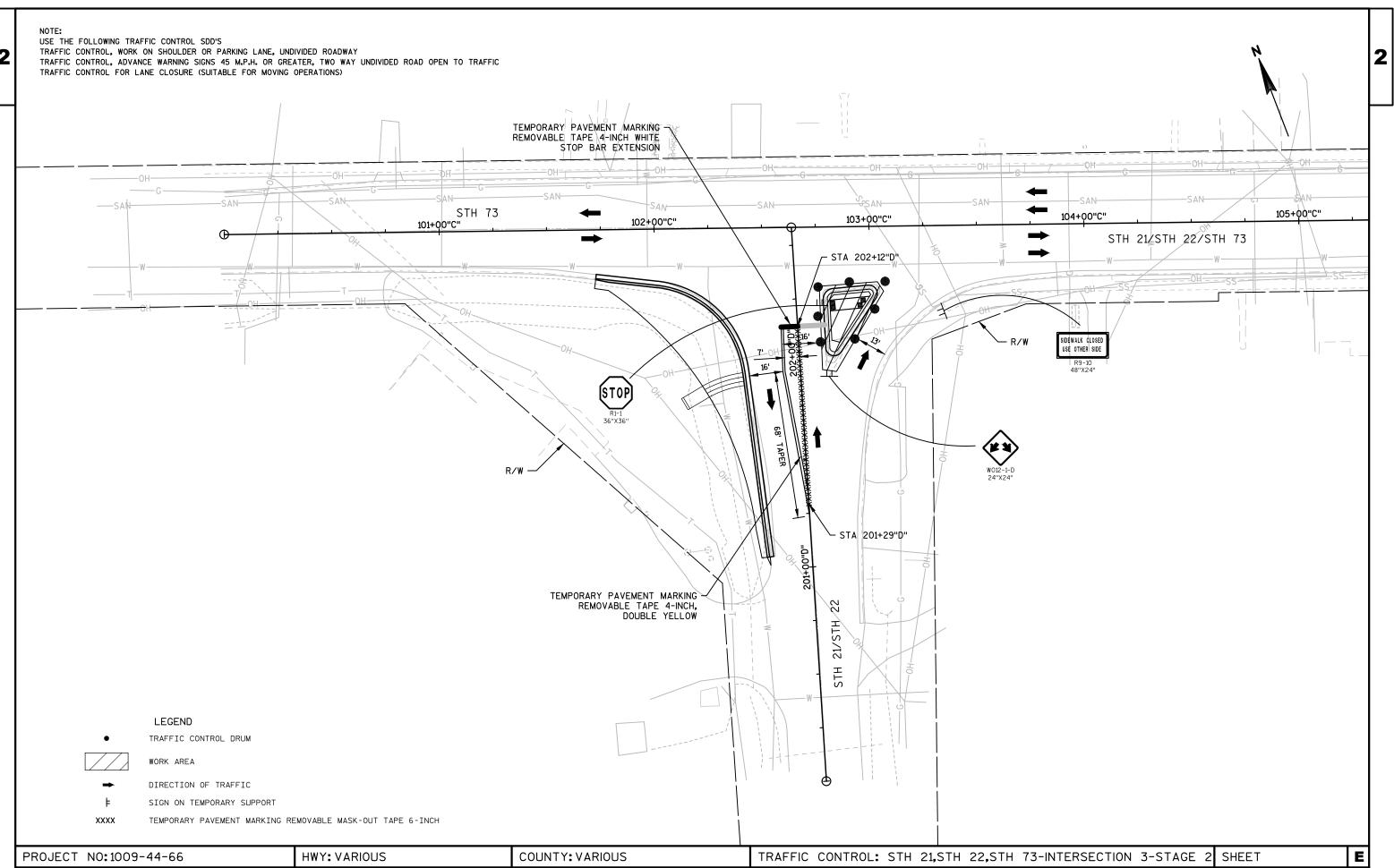


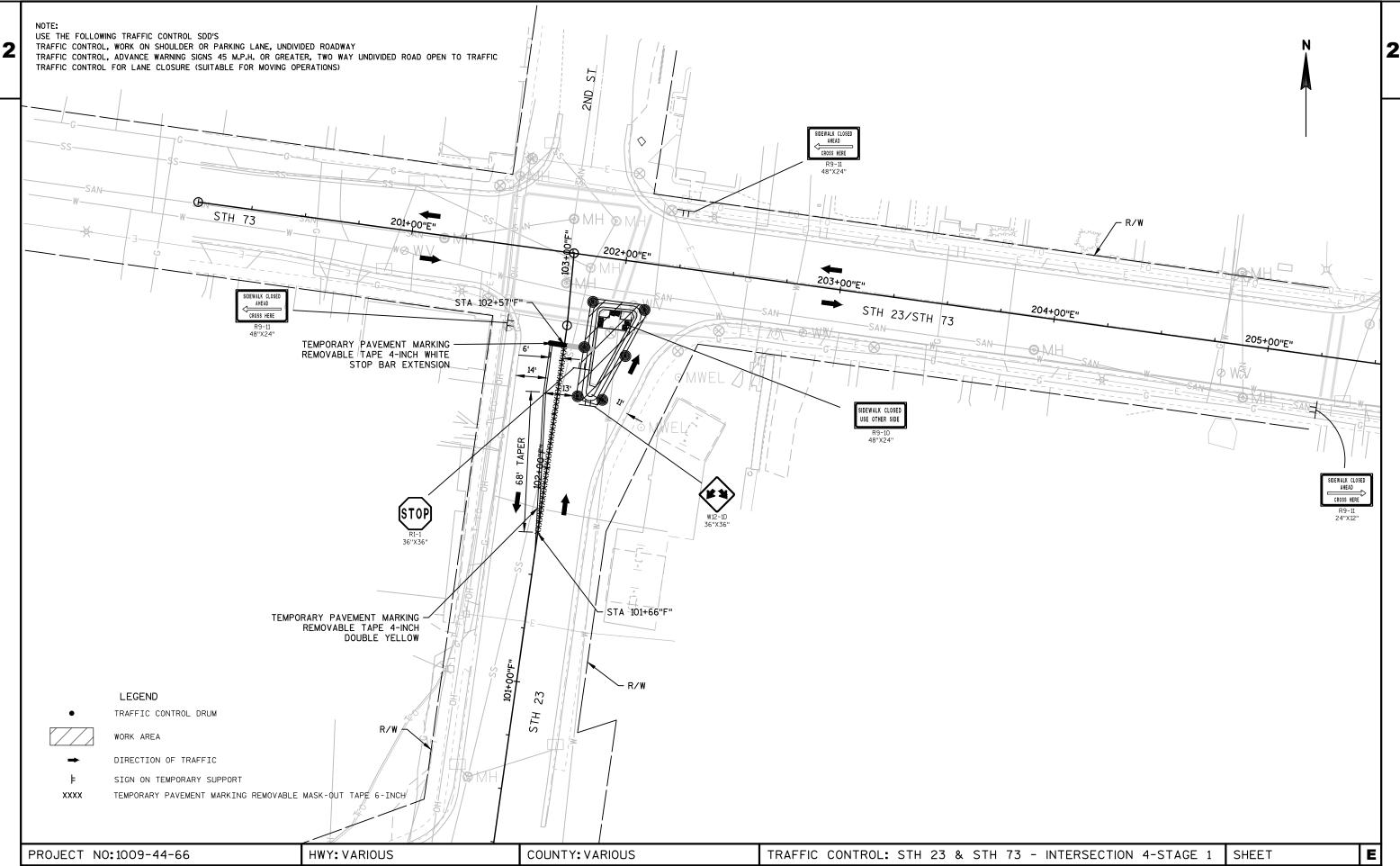


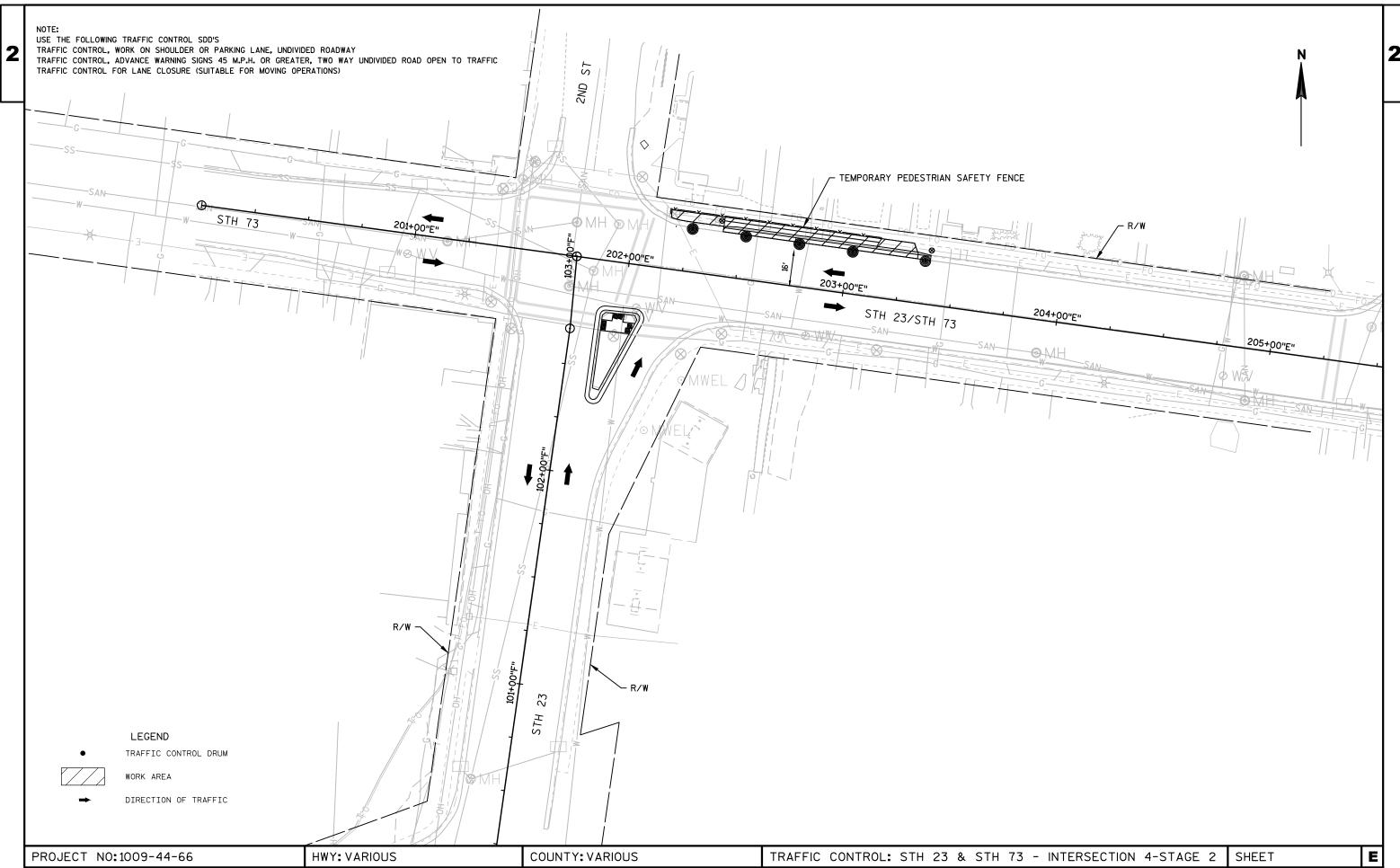


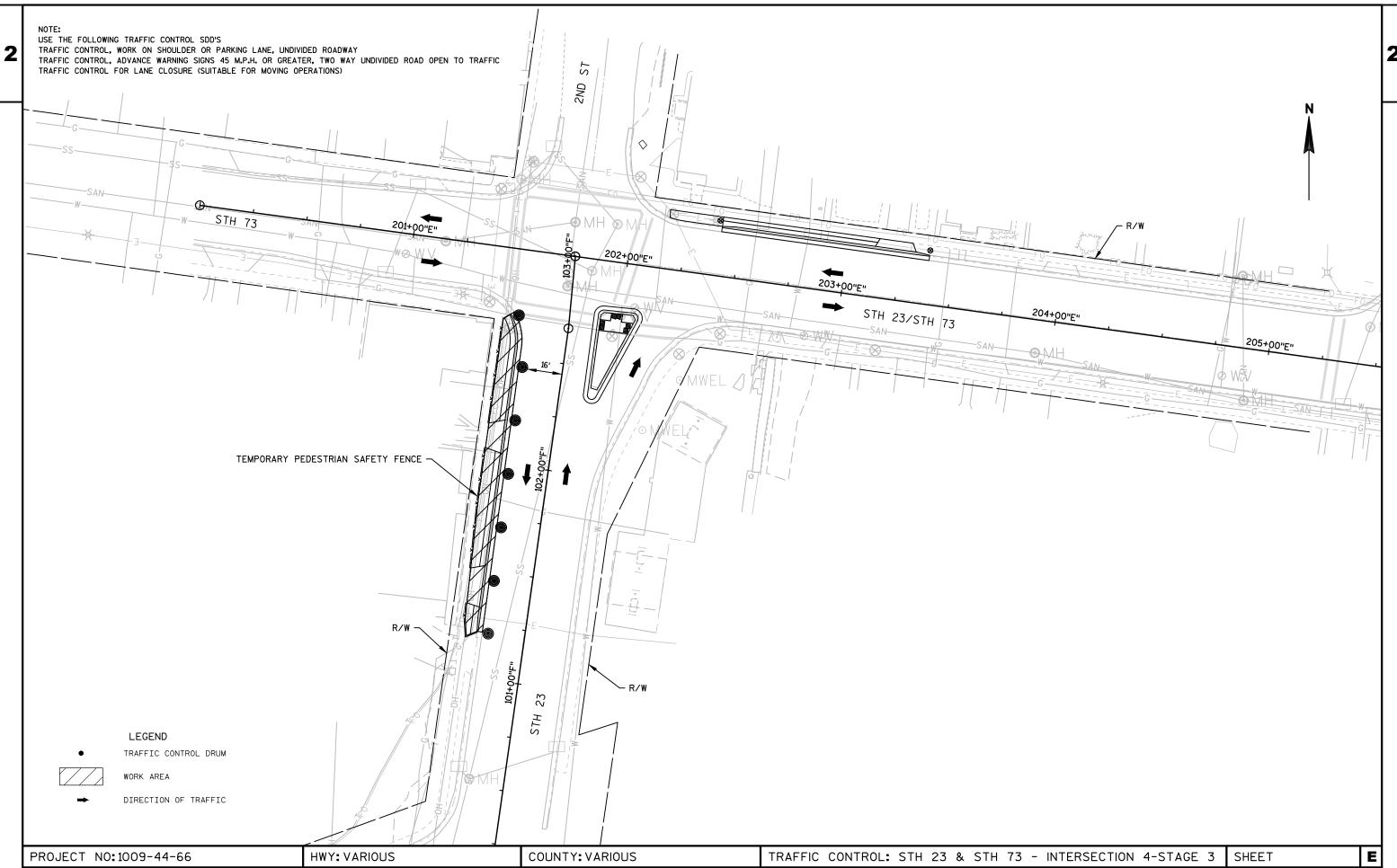












DATE 21	MAR16	E S T	TIMATE	OF QUANT		
LI NE NUMBER	LTEM	LTEM DECODEDITION	LINII T	TOTAL	1009-44-66	
	ITEM 201.0120	I TEM DESCRIPTION	UNIT ID	TOTAL 12. 000	QUANTI TY	
0010 0020	201. 0120	CI eari ng Grubbi ng	I D	12. 000	12. 000 12. 000	
	201. 0220		SY	66. 000	66. 000	
0030	204. 0100	Removing Pavement	SY	545. 000	545. 000	
0040		Removing Asphaltic Surface	LF			
0050	204. 0150	Removing Curb & Gutter	LF	1, 125. 000	1, 125. 000	
0060	204. 0155	Removing Concrete Sidewalk	SY	40. 000	40. 000	
0070	204. 0190	Removing Surface Drains	EACH	1. 000	1. 000	
0800	204. 0220	Removing Inlets	EACH	2. 000	2. 000	
0090	204. 0245	Removing Storm Sewer (size) 01. 12-Inch	LF	4. 000	4. 000	
0100		Grading and Shaping Intersection	LS	1. 000	1. 000	
0100	203. 9010. 3	(location) 01. STH 21 & IH 39 SB Off	LJ	1.000	1.000	
		Ramp - Intersection 1				
		Kamp - Titter Section 1				
0110	205 9010 S	Grading and Shaping Intersection	LS	1. 000	1. 000	
0110	200. 7010. 0	(location) 02. STH 21 & STH 22 -	20	1.000	1.000	
		Intersection 2				
0120	205 9010 S	Grading and Shaping Intersection	LS	1.000	1. 000	
0120	200. 7010. 0	(location) 03. STH 21, STH 22 & STH 73		1.000	1.000	
		- Intersection 3				
0130	205 9010 9	Grading and Shaping Intersection	LS	1. 000	1. 000	
3130	200. 7010. J	(location) 04. STH 23 & STH 73 -		1.000	1.000	
		Intersection 4				
0140	213. 0100	Finishing Roadway (project) 01.	EACH	1. 000	1. 000	
0140	213.0100	1009-44-66	LACIT	1.000	1.000	
0150	305. 0110	Base Aggregate Dense 3/4-Inch	TON	657.000	657. 000	
0100	000.0110	base riggi egate belise of i mon	1011	007.000	007.000	
0160	405. 0100	Coloring Concrete Red	CY	419. 000	419, 000	
0170	415. 0080	Concrete Pavement 8-Inch	SY	106. 000	106. 000	
0180	415. 0120	Concrete Pavement 12-Inch	SY	1, 321. 000	1, 321. 000	
0190	416. 0610	Drilled Tie Bars	EACH	168. 000	168. 000	
0200	416. 1010	Concrete Surface Drains	CY	6. 000	6. 000	
0210	455. 0605	Tack Coat	GAL	43.000	43.000	
0220	465. 0105	Asphaltic Surface	TON	128. 000	128. 000	
0230	465. 0315	Asphaltic Flumes	SY	13.000	13.000	
0240	520. 8700	Cleaning Culvert Pipes	EACH	1.000	1. 000	
0250	601. 0405	Concrete Curb & Gutter 18-Inch Type A	LF	39.000	39.000	
0260	601.0580	Concrete Curb & Gutter 4-Inch Sloped	LF	1, 369. 000	1, 369. 000	
		36-Inch Type R				
0270	602.0405	Concrete Sidewalk 4-Inch	SF	254.000	254.000	
0280	602.0505	Curb Ramp Detectable Warning Field	SF	40.000	40.000	
		Yellow				
0290	608. 0312	Storm Sewer Pipe Reinforced Concrete	LF	4.000	4.000	
		Class III 12-Inch				
0300	611. 0652	Inlet Covers Type T	EACH	2.000	2. 000	
	/44 22==					
0310	611. 3225	Inlets 2x2.5-FT	EACH	2.000	2. 000	
0320		Fence Safety	LF	1, 573. 000	1, 573. 000	
0330	619. 1000	Mobilization	EACH	1.000	1.000	
0340	620. 0300	Concrete Median Sloped Nose	SF	275. 000	275. 000	
0350	624. 0100	Water	MGAL	9. 800	9. 800	
	, of 6100	-		4 444 222	4 444 222	
0360	625. 0100	Topsoi I	SY	1, 411. 000	1, 411. 000	
0370	627. 0200	Mul chi ng	SY	1, 411. 000	1, 411. 000	
0380	628. 1905	Mobilizations Erosion Control	EACH	4. 000	4. 000	
0390	628. 1910	Mobilizations Emergency Erosion Control	EACH	8. 000	8. 000	
0400	628. 2008	Erosion Mat Urban Class I Type B	SY	1, 479. 000	1, 479. 000	
0410	/20 7045	Inlat Destantion Type C		7 000	7.000	
0410	628. 7015	Inlet Protection Type C	EACH	7.000	7. 000	
0420	628. 7504	Temporary Ditch Checks	LF	145. 000	145. 000	
0430	628. 7555	Culvert Pipe Checks	EACH	4.000	4. 000	
0440	629. 0210	Fertilizer Type B	CWT	0. 640	0. 640	

DATE 21 LINE	IMAR16	EST	IMAT	E OF QUAN	T I T I E S 1009-44-66
NUMBER 0450	ITEM 630.0120	ITEM DESCRIPTION Seeding Mixture No. 20	UNI T LB	TOTAL 28. 000	QUANTI TY 28. 000
0460	630. 0140	Seeding Mixture No. 40	LB	9. 100	9. 100
0470	631. 0300 633. 0200	Sod Water	MGAL	35. 650	35. 650
0480 0490	634. 0616	Delineators Flexible Posts Wood 4x6-Inch X 16-FT	EACH EACH	12. 000 7. 000	12. 000 7. 000
0500	634. 0812	Posts Tubular Steel 2x2-Inch X 12-FT	EACH	16. 000	16. 000
0510	637. 2210	Signs Type II Reflective H	SF	203. 800	203. 800
0520	637. 2230	Signs Type II Reflective F	SF	4.000	4. 000
0530 0540	638. 2102 638. 2602	Moving Signs Type II Removing Signs Type II	EACH EACH	7. 000 22. 000	7. 000 22. 000
0550	638. 3000	Removing Small Sign Supports	EACH	26. 000	26. 000
0560	642. 5001	Field Office Type B	EACH	1.000	1. 000
0570	643. 0100	Traffic Control (project) 01. 1009-44-66	EACH	1.000	1.000
0580 0590	643. 0300 643. 0410	Traffic Control Drums Traffic Control Barricades Type II	DAY DAY	1, 376. 000 140. 000	1, 376. 000 140. 000
0600	643. 0420	Traffic Control Barricades Type III	DAY	14. 000	14. 000
0610 0620	643. 0705 643. 0715	Traffic Control Warning Lights Type A Traffic Control Warning Lights Type C	DAY DAY	14. 000 42. 000	14. 000 42. 000
0630	643. 0900	Traffic Control Signs	DAY	2, 115. 000	2, 115. 000
0640		S Temporary Pedestrian Safety Fence	LF	224. 000	224. 000
0650	646. 0106	Pavement Marking Epoxy 4-Inch	LF	974. 000	974. 000
0660	646. 0126	Pavement Marking Epoxy 8-Inch	LF	421. 000	421. 000
0670	647. 0456	Pavement Marking Curb Epoxy	LF	88.000	88. 000
0680 0690	647. 0606 649. 0400	Pavement Marking Island Nose Epoxy Temporary Pavement Marking Removable	EACH LF	1. 000 1, 936. 000	1. 000 1, 936. 000
		Tape 4-Inch			
0700	649. 0506	Temporary Pavement Marking Removable Mask-Out Tape 6-Inch	LF	1, 014. 000	1, 014. 000
0710	650. 4000	Construction Staking Storm Sewer	EACH	2.000	2. 000
0720	650. 4500	Construction Staking Subgrade	LF	1, 129. 000	1, 129. 000
0730	650. 5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1, 440. 000	1, 440. 000
0740	650. 7000	Construction Staking Concrete Pavement	LF	897. 000	897. 000
0750	650. 9910	Construction Staking Supplemental Control (project) 01. 1009-44-66	LS	1. 000	1. 000
0760	650. 9920	Construction Staking Slope Stakes	LF	624. 000	624. 000
0780	652. 0225	Conduit Rigid Nonmetallic Schedule 40	LF	100. 000	100. 000
		2-I nch			
0780	653. 0115	Pull Boxes Steel 12x36-Inch	EACH	2.000	2. 000
0790 0800	653. 0900 653. 0905	Adjusting Pull Boxes Removing Pull Boxes	EACH EACH	4. 000 2. 000	4. 000 2. 000
0810	690. 0150	Sawing Asphal t	LF	1, 054. 000	1, 054. 000
0820	690. 0250 715. 0415	Sawing Concrete Incentive Strength Concrete Pavement	LF DOI	345. 000 500. 000	345. 000 500. 000
0830 0840	SPV. 0030	Special 01. Fertilizer for Lawn Type	DOL CWT	0. 314	0. 314
		Turf			
0850	SPV. 0090	Special 01. Concrete Curb & Guttter 30-Inch Type A Modified	LF	71. 000	71. 000
0860	SPV. 0090	Special O2. Furnish and Install	LF	1, 540. 000	1, 540. 000
		Equivalent Lighting Conductors			
0870	SPV. 0105	Special 01. Concrete Pavement Joint Layout	LS	1. 000	1. 000
0880	SPV. 0180	Special 01. Preparing Topsoil for Lawn	SY	498. 000	498. 000
		Type Turf			

	3

			REMOVING A	ASPHALTIC SU	<u>JRFACE</u>			REM	OVING CURB & G	<u>UTTER</u>	
CATEG	ORY STA	TION - STATION	LOCA	ATION	204.0110 SY	REMARKS	CATEGOR	Y STATION - STATION	LOCATION	204.0150 LF	REMARKS
10 10 10 10 10 10	18+60 17+95 195+1 194+8	ND - 20+12ND ND - 20+02ND ND - 18+35ND 6N - 195+25N 1N - 195+16N - 0+92Z	LT LT ISLAND MEDIAN ISLAI LT RT	ND	71 48 21 8 6 50	5' SHOULDER 3' SHOULDER ISLAND PAVEMENT MEDIAN ISLAND PAVEMENT ASPHALT IN FRONT OF GUTTER STH 21 WB SHOULDER	10 10 10	0+92Z - 1+76Z 17+95ND - 18+35ND 194+81N - 195+32N	NW RADIUS ISLAND MEDIAN ISLAND CAT 10 SUBTOTA		
	V	0 022		10 SUBTOTAL	204		20 20	2+80Y - 3+77Y 202+14B - 202+77B	NW RADIUS ISLAND	97 145	
20 20 20 30 30 30 40 40 40	202+1 2+27Y 201+9 201+9 1+10X 202+4 101+2	4B - 202+77B 4B - 202+77B - 3+87Y 3D - 202+27D 3D - 202+27D - 2+92X 1E - 203+38E 1F - 102+73F 4F - 102+78F	ISLAND ISLAND RT CAT LT LT ISLAND	20 SUBTOTAL 30 SUBTOTAL 40 SUBTOTAL TOTAL	34 15 37 86 22 75 42 139 22 34 60 116	2' IN FRONT OF GUTTER ISLAND PAVEMENT 2' IN FRONT OF GUTTER 2' IN FRONT OF GUTTER ISLAND PAVEMENT 2' IN FRONT OF GUTTER 2' IN FRONT OF GUTTER 2' IN FRONT OF GUTTER 2' IN FRONT OF GUTTER 2' IN FRONT OF GUTTER	30 30 40 40 40	1+12X - 2+92X 201+93D - 202+27D 202+41E - 203+38E 101+21F - 102+73F 102+34F - 102+78F	CAT 20 SUBTOTA SW RADIUS ISLAND CAT 30 SUBTOTA LT LT ISLAND CAT 40 SUBTOTA TOTA	180 98 AL 278 97 152 100 AL 349	STH 23/STH 73 STH 23
			CLEARING	& GRUBBING	<u> </u>			REM	OVING PAVEMEN	<u>T</u>	
	CATEGORY	STATION	LOCATION	201.0120 CLEARING ID	201.0220 GRUBBING ID	REMARKS	CATEGORY	STATION - STATION	WIDTH 204.0 FEET SY		EMARKS
	40	101+99F	LT 40 SUBTOTAL	12	12	NEW HAR	10 10	195+16N - 195+25N 194+81N - 195+32N 19+51ND - 19+63ND	3 3 0 - 12 28 0 - 2 1		ISTALLATION TAPER
		TOTALS		12	12		40 40 40	203+12E - 203+32E 101+33F - 101+52F 102+05F - 102+21F	SUBTOTAL 32 4.5 9 7.5 12 7.5 13 SUBTOTAL 34	DRIVEW DRIVEW	/AY APRON /AY APRON /AY APRON
DD0::50	T.NO. 4222	44.00	1,040	. VA DIOLIC		OOUNTY MADIONS	I MIGOSTI I MISONO	OHANITITIES	TOTAL 66		Tourer
PROJEC	T NO: 1009-	44-00	HWY:	: VARIOUS		COUNTY: VARIOUS	MISCELLANEOUS	QUANTITIES			SHEET:

GRADING & SHAPING INTERSECTION

		*COMMON	*===	205 0040 0	
CATEGORY	LOCATION	EXCAVATION CY	*FILL CY	205.9010.S LS	REMARKS
10	STH 21 & IH 39 SB OFF RAMP - INTERSECTION 1	532	171	1	
	CAT 10 SUBTOTAL	532	171	1	
20	STH 21 & STH 22 - INTERSECTION 2	99	73	1	
	CAT 20 SUBTOTAL	99	73	1	
30	STH 21, STH 22 & STH 73 - INTERSECTION 3	205	14	1	
	CAT 30 SUBTOTAL	205	14	1	
40	STH 23 & STH 73 - INTERSECTION 4	113	0	1	
	CAT 40 SUBTOTAL	113	0	1	
	TOTALS	949	258	4	

*QUANTITIES ARE GIVEN FOR INFORMATION ONLY AND ARE INCLUDED UNDER THE BID ITEM GRADING AND SHAPING INTERSECTION EXPANSION FACTOR = 1.3

REMOVING INLETS & STORM SEWER

CATEGORY	STATION	LOCATION	204.0220 REMOVING INLETS EACH	204.0245 REMOVING STORM SEWER (12-INCH) LF	REMARKS
10 10	195+22N 195+22N	CL LT	1 1	 4	STORM SEWER CONNECTS INLETS
	CAT	10 SUBTOTAL	2	4	=
		TOTAL	2	4	=

BASE AGGREGATE DENSE & WATER

305.0110 BASE AGGREGATE DENSE 624 3/4 INCH WA

			DENSE 3/4 INCH	624.0100 WATER	
CATEGORY	STATION - STATION	LOCATION	TON	MGAL	REMARKS
CATEGORT	STATION - STATION	LOCATION	ION	WIGAL	REWARRS
10	0+22Z - 3+00Z	LT	239	3.6	
10	194+81N - 195+24N	MEDIAN ISLAND	15	0.2	MEDIAN ISLAND
10	17+98ND - 18+28ND	ISLAND	9	0.1	ISLAND
10	18+60ND - 20+02ND	RT	4		RIGHT SHOULDER
10	19+79ND - 20+12ND	LT	3		LEFT SHOULDER
	CAT 10 SUBTOTAL		269	3.9	
20	2+38Y - 3+82Y	LT	89	1.3	
20	202+17B - 202+70B	ISLAND	35	0.5	ISLAND
	CAT 20 SUBTOTAL		124	1.9	
30	1+12X - 2+92X	LT	124	1.9	
30	202+02D - 202+26D	ISLAND	21	0.3	ISLAND
	CAT 30 SUBTOTAL		146	2.2	
40	202+16E - 202+41E	LT	1	0.1	
40	202+41E - 203+38E	LT	34	0.5	
40	101+21F - 102+73F	LT	56	0.8	
40	102+34F - 102+78F	ISLAND	26	0.4	ISLAND
	CAT 40 SUBTOTAL		118	1.8	
	TOTAL		657	9.8	

REMOVING CONCRETE SIDEWALK

N LOCATION	204.0155 SY	REMARKS
LT SLAND	2 39	TERRACE
CAT 40 SUBTOTAL	40	
TOTAL	40	
	LT ISLAND CAT 40 SUBTOTAL	LT 2 ISLAND 39 CAT 40 SUBTOTAL 40

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

			CONCRETE PAVEM	ENT & COLOR	RING CONCRI	ETE RED		DRILLED TIE BARS
				415.0080 CONCRETE PAVEMENT 8-INCH	415.0012 CONCRETE PAVEMENT 12-INCH	405.0100 COLORING CONCRETE RED		CATEGORY STATION - STATION LOCATION EACH 10 0+22Z - 3+00Z NW RADIUS 93
┪	CATEGORY	STATION - STATION	LOCATION	SY	SY	CY	REMARKS	10 194+81N - 195+32N MEDIAN ISLAND 41
3	10 10 10 10	0+22Z -0+70Z 0+22Z - 2+96Z 2+19Z - 3+00Z 17+98ND - 18+28ND	RT LT RT ISLAND	24 53 	 640 16	211 5	CONCRETE SHOULDER CONCRETE SHOULDER ISLAND PAVEMENT	10 17+95ND - 18+35ND ISLAND 34 CAT 10 SUBTOTAL 168 TOTAL 168
┫	10 10	194+81N - 195+24N	MEDIAN ISLAND	 26	42	14	MEDIAN ISLAND PAVEMENT	REMOVING SURFACE DRAINS
	10 10	194+97N - 195+32N 195+16N - 195+25N	LT RT	26 3			PAVEMENT PATCHING PAVEMENT PATCHING	REWICKING SURFACE DRAINS
	10	1931 1014 - 1931 2314	IXI	3			TAVEMENTTATORING	204.0190
			CAT 10 SUBTOTAL	106	698	230		CATEGORY STATION LOCATION EACH REMARKS
	20	2+38Y - 3+82Y	LT		176	58		10 18+60ND LT 1
	20	202+17B - 202+70B	ISLAND		51	17	ISLAND PAVEMENT	<u></u>
			CAT 20 SUBTOTAL	0	227	75		CAT 10 SUBTOTAL 1
	20	4 : 40 / . 0 : 00 /			040	70		TOTAL 1
	30 30	1+12X - 2+92X 202+02D - 202+26D	LT ISLAND		212 21	70 7		
	30	202+02D - 202+26D 202+21D	ISLAND		9	<i>/</i> 	ISLAND SIDEWALK	ASPHALTIC FLUMES
	00	202.210	IOL/ IIID		Ŭ		ICE THE CIBEWALK	
			CAT 30 SUBTOTAL	0	242	77		465.0315 CATEGORY STATION LOCATION SY REMARKS
	40	202+41E - 203+12E	LT		20	7	STH 23/STH 73	
	40	203+12E - 203+32E	LT		9		DRIVEWAY APRON	10 2+96Z LT 5
	40	101+21F - 101+33F	LT		8	3	STH 23	CAT 10 SUBTOTAL 5
	40	101+33F - 101+52F	LT		12		DRIVEWAY APRON	CAT TO SOBTOTAL 5
	40	101+52F - 102+05F	LT		42	14	STH 23	30 1+96X LT 8
	40 40	102+05F - 102+21F 102+21F - 102+70F	LT LT		11 13	4	DRIVEWAY APRON STH 23	
1	40 40	102+21F - 102+70F 102+41F - 102+75F	ISLAND		29	10	ISLAND PAVEMENT	CAT 30 SUBTOTAL 8
	40	102+69F	ISLAND		10		ISLAND SIDEWALK	
	.0	102 / 001					IOD WID CIDEWN LERC	TOTAL 13
			CAT 40 SUBTOTAL	0	154	37		CONCRETE SURFACE DRAIN
			TOTAL	106	1321	419		416 1010
ľ			CONCRETE SIDEWAL	<u>.K</u>			CATEGORY	416.1010 Y STATION - STATION CY REMARKS
					0405 NCH		10	2+92Z - 3+00Z 2 END OF CURB & GUTTER
		CATEGORY	STATION - STATION LOG		SF			CAT 10 SUBTOTAL 2
			202+16E - 202+41E LT		06		30	1+96X 4 LOCATED WITHIN CONCRETE PAVEMENT 12-INCH
		40	102+21F - 102+68F LT	1	48			CAT 20 CURTOTAL
			CAT 40 SU	BTOTAL 2	54			CAT 30 SUBTOTAL 4
					 54			TOTAL 6
H	PROJECT NO	. 1000-44 66	HWY: VARIOUS			r: VARIOUS	MISCELLANICO	DUS QUANTITIES SHEET: E
	ILE NAME : 1009-44-66		I HW 1. VARIOUS		COUNT			ACH GRULING PLOT NAME: PLOT SCALE: 1:1

4)
•	3

			ASPHALTIC SUR	FACE & TACK	COAT			co	NCRETE CURB & G	UTTER		
	CATEGORY	STATION - STATION	LOCATION	465.0105 ASPHALTIC SURFACE TON	455.0605 TACK COAT GAL	REMARKS	CATEGORY	STATION - STATION	LOCATION	601.0405 18-INCH TYPE A LF	601.0580 36-INCH TYPE R LF	SPV.0090.01 30-INCH TYPE A MODIFIED LF
	10	18+60ND - 20+02ND	RT	55	25	8-INCH DEPTH SHOULDER	10	0+22Z - 2+96Z	LT		274	
	10	19+79ND - 20+12ND	LT	4	2	3.5-INCH DEPTH SHOULDER	10	17+95ND - 18+35ND	ISLAND		274 81	
i				•	_	0.0 0 22 0 0.2222	10	194+81N - 195+32N	MEDIAN ISLAND		101	
ĺ			CAT 10 SUBTOTAL	60	27		10	194+01IN - 193+32IN	WEDIAN ISLAND		101	
									CAT 10 SUBTOTAL	0	456	0
1	20	2+27Y - 3+87Y	RT	13	3	5.5-INCH DEPTH			ON TO COBTOTAL	Ü	100	Ü
	20	202+14B - 202+77B	ISLAND	11	2	5.5-INCH DEPTH	20	2+38Y - 3+82Y	LT		144	
							20	202+14B - 202+77B	ISLAND		150	
			CAT 20 SUBTOTAL	23	5							
									CAT 20 SUBTOTAL	0	294	0
	30	1+10X - 2+92X	RT	13	3	5.5-INCH DEPTH						
	30	201+93D - 202+27D	ISLAND	8	2	5.5-INCH DEPTH	30	1+12X - 2+92X	LT		180	
			CAT OO CUIDTOTAL				30	201+93D - 202+27D	ISLAND		87	
			CAT 30 SUBTOTAL	21	5							
	40	202+41E - 203+38E	LT	6	2	5-INCH DEPTH			CAT 30 SUBTOTAL	0	267	0
	40	101+21F - 102+73F	LT	10	2	5-INCH DEPTH	40	000:445 000:405				- .
	40	102+34F - 102+78F	ISLAND	8	2	5-INCH DEPTH	40	202+41E - 203+12E	LT			71
	10	102 10 102 101	1011 1111	Ü	_	0 111011 1111	40	202+41E - 203+38E 101+21F - 102+73F	LT LT		97 152	
			CAT 40 SUBTOTAL	24	6		40 40	101+21F - 102+73F 101+21F - 102+60F	LT LT	 39		
					-		40	102+34F - 102+78F	ISLAND	39	103	
			TOTAL	128	42		40	1021341 - 1021761	ISLAND		103	
									CAT 40 SUBTOTAL	39	352	71
									TOTAL	39	1369	71

CLEANING CULVERT PIPES

			520.87	
CATEGORY	STATION	LOCATION	EACH	REMARKS
10	195+02N	RT	1	12 INCH PIPE 65 FEET LONG
	CAT	10 SUBTOTAL	1	
		TOTAL	1	

STORM SEWER STRUCTURES & STORM SEWER

					608.0312	
					STORM	
					SEWER PIPE	
				611.0652	REINFORCED	
			611.3225	INLET	CONCRETE	
			INLETS	COVERS	CLASS III	
			2X2.5-FT	TYPE T	12-INCH	
CATEGORY	STATION	LOCATION	EACH	EACH	LF	REMARKS
10	195+22N	LT	2	2	4	PIPE FOR CONNECTING INLETS
	CAT 10	SUBTOTALS	2	2	4	
	CAT 10			<u>-</u>	·	
	CAT 10	SUBTOTALS TOTALS	2	2	4	

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

CONCRETE MEDIAN SLOPED NOSE

CATEGORY	STATION	LOCATION	620.0300 SF	REMARKS
10 10	18+32ND 195+27N	LT LT	47 74	TYPE 1 TYPE 1
	CAT 10 SUBTOTAL		121	
20	202+72B	LT	52	TYPE 1
	CAT 20 SUBTOTAL		52	
30	202+02D	RT	52	TYPE 1
	CAT 30 SUBTOTAL		52	
40	102+38F	RT	50	TYPE 1
	CAT 40 SUBTOTAL		50	
		TOTAL	275	

MOBILIZATIONS EROSION CONTROL

CATEGORY	STATION - STATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	REMARKS
10	0+22Z - 3+25Z	1	2	INTERSECTION 1
	CAT 10 SUBTOTAL	1	2	
20	2+25Y - 4+00Y	1	2	INTERSECTION 2
	CAT 20 SUBTOTAL	1	2	
30	1+00X - 2+92X	1	2	INTERSECTION 3
	CAT 30 SUBTOTAL	1	2	
40	201+25E - 205+50E, 100+50F - 103+00F	1	2	INTERSECTION 4
	CAT 40 SUBTOTAL	1	2	
	TOTALS	4	8	

EROSION MAT URBAN CLASS I TYPE B

CATEGORY	STATION	LOCATION	628.2008 SY	REMARKS
10 10	0+22Z - 3+00Z UNDISTRIBUTEI	n	913 42	
10	ONDIO INIDOTEI	CAT 10 SUBTOTAL	955	
20	2+10Y - 3+95Y		220	
20	UNDISTRIBUTE		11	
30	1+00X - 2+92X	CAT 20 SUBTOTAL	231 272	
30	UNDISTRIBUTE	D	14	
		CAT 30 SUBTOTAL	286	
40 40	203+31E - 203+4 UNDISTRIBUTEI		6 1	
		CAT 40 SUBTOTAL	7	
		TOTALS	1479	

FENCE SAFETY & TEMPORARY PEDESTRIAN SAFETY FENCE

CATEGORY	STATION - STATION	LOCATION	616.0700.S FENCE SAFETY LF	644.1616.S TEMPORARY PEDESTRIAN SAFETY FENCE LF	REMARKS
10	IH 39 SB OFF RAMP	LT	793		
10	IH 39 SB OPF RAMP	LT	793 780		
		CAT 10 SUBTOTAL	1573	0	
40	202+16E - 203+14E	LT		100	STAGE 2
40	101+18F - 101+33F	LT		52	STAGE 3
40	101+52F - 102+05F	LT		56	STAGE 3
40	102+21F - 102+70F	LT		16	STAGE 3
		CAT 40 SUBTOTAL	0	224	
		TOTAL	1573	224	

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

TOPSOIL, MULCHIN	G, FERTILIZER,	SEEDING	, AND PREPAIRING TOPSOIL FOR LAWN T	YPE TURF

CATEGORY	STATION - STATION	N LOCATION	*625.0100 TOPSOIL SY	627.0200 MULCHING SY	629.0210 FERTILIZER TYPE B CWT	630.0120 SEEDING MIXTURE NO. 20 LB	631.0300 SOD WATER MGAL	630.0140 SEEDING MIXTURE NO. 40 LB	SPV.0030.01 FERTILIZER FOR LAWN TYPE TURF CWT	SPV.0180.01 PREPAIRING TOPSOIL FOR LAWN TYPE TURF SY	REMARKS
10	0+22Z - 3+00Z	LT	913	913	0.58	25	20.5				NW DITCH
10	18+60ND - 20+02ND	RT	915	915	0.07	3	20.5				RAMP SHOULDER
10	UNDISTRIBUTED	131					2.3				TO THE OTHER PROPERTY.
		CAT 10 SUBTOTAL	913	913	0.64	28	23	0	0	0	=
20	2+10Y - 3+95Y	LT	220	220			4.9	4	0.14	220	NW DITCH
20	UNDISTRIBUTED						0.6				
		CAT 20 SUBTOTAL	220	220	0	0	5.5	4	0.14	220	<u> </u>
30	1+00X - 2+92X	LT	272	272			6.1	5	0.17	272	SW DITCH
30	UNDISTRIBUTED						0.9				
		CAT 30 SUBTOTAL	272	272	0	0	7	5	0.17	272	_
40	203+31E - 203+40E	LT	6	6			0.1	0.1	0.004	6	
40	UNDISTRIBUTED						0.05				
		CAT 40 SUBTOTAL	6	6	0	0	0.15	0.1	0.004	6	=
		TOTALS	1411	1411	0.64	28	35.5	9	0.31	498	_

^{* 6 -} INCH THICK TOPSOIL

<u>INLET</u>	PROTECTION TYPE C

CATEGORY	STATION	LOCATION	628.7015 EACH
10	195+22N	LT	2
	CAT	10 SUBTOTAL	2
30	202+17D	RT	1
	CAT	30 SUBTOTAL	1
40	205+25E	RT	1
40	205+55E	LT	1
40	100+57F	RT & LT	2
	CAT	40 SUBTOTAL	4
		TOTALS	7

CULVERT PIPE DITCH CHECKS

CATEGORY	STATION	LOCATION	628.7555 EACH
30	200+87D	LT	4
	CAT	30 SUBTOTAL	4
		TOTAL	4

CURB RAMP WARNING DETECTION FIELD

CATEGORY	STATION	LOCATION	602.0505 SF	REMARKS
30	202+21D	16' RT	8	ISLAND
30	202+21D	30' RT	8	ISLAND
		CAT 30 SUBTOTAL	16	
40	201+98E	RT	8	ISLAND
40	102+69F	14' RT	8	ISLAND
40	102+69F	26' RT	8	ISLAND
		CAT 40 SUBTOTAL TOTAL	24	

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: E
FILE NAME: 1009-44-66 misc_q.ppt PLOT NAME: ______ PLOT SCALE: 1:1

634.0616 634.0812 638.3000 **POSTS** POSTS 637.2210 637.2230 638.2102 638.2602 REMOVING TUBULAR SIGNS MOVING **REMOVING** WOOD SIGNS SMALL STEEL TYPE II TYPE II SIGNS SIGNS 4X6-INCH SIGN SIGN SIGN TYPE II TYPE II **SUPPORTS** 16-FT 2"X2"X12-FT REFLECTIVE H REFLECTIVE F CATEGORY NO. CODE MESSAGE SIZE EACH EACH SF SF EACH EACH EACH 10 1-01 J2-2 48" X 57" 19.00

24" X 12"

M3-2

M3-2

M3-4

M1-6

10

10

10

10

EAST

EAST

WEST

STH 21

	_	_	=							
1	10	M3-4	WEST	24" X 12"	 	 				
1	10	M1-6	STH 21	24" X 24"	 	 				_
1	10	M1-6	STH 21	24" X 24"	 	 				
1	10	M6-1	RIGHT ARROW	21" X 21"	 	 				
1	10	M6-1	LEFT ARROW	21" X 21"	 	 				
1	1-02	W12-1	D DOUBLE DOWN ARROW	24" X 24"	 1	 	1		1	
1	1-03	R1-1	STOP	36" X 36"	 1	 	1		1	
1	1-04	R6-3	DIVIDED HIGHWAY	30" X 24"	 	 	1			MOUNT ON SAME POST AS 1-03
1	1-05	R5-1	DO NOT ENTER	36" X 36"	 	 	1			MOUNT TO BACK OF 1-03
1	1-06	R1-2	YIELD	36" X 31"	 1	 	1		1	
1	1-07	R5-1	DO NOT ENTER	36" X 36"	 1	 	1		1	MOUNT TO BACK OF 1-06
1	1-08	R3-2	NO LEFT TURN	24" X 24"	 1	 	1		1	
1	1-09	R6-2-L	ONE WAY	24" X 30"	 	 		1	1	REMOVE ONLY
1	1-10	J2-2		48" X 57"	 	 		1	1	REMOVE ONLY

SIGN LISTING

REMARKS

10		M1-6	STH 21								
10		M6-1	RIGHT ARROW								
10		M6-1	LEFT ARROW								
			CA	T 10 SUBTOTAL	1	5	19.00	0	7	2	7
20	2-01	R1-1	STOP	36" X 36"		1	7.46			1	1
20	2-02	R1-3-P	ALL WAY	24" X 9"			1.50			1	
20	2-03	W12-10	DOUBLE DOWN ARROW	24" X 24"		1		4.00		1	1
20	2-04	R1-2	YIELD	36" X 31"		1				1	1
			CA	T 20 SUBTOTAL	0	3	8.96	4.00	0	4	3

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

SIGN LISTING

CATEGORY	SIGN NO.	SIGN CODE		SIZE	634.0616 POSTS WOOD 4X6-INCH 16-FT EACH	634.0812 POSTS TUBULAR STEEL 2"X2"X12-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2102 MOVING SIGNS TYPE II EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
30	3-01	J2-4		96" X 57"	2		38.00			1	2	
30	3-01	M3-2	EAST	24" X 12"			30.00					
30		M3-1	NORTH	24" X 12"								
30		M3-4	WEST	24" X 12"								
30		M3-3	SOUTH	24" X 12"								
30		M1-6	STH 21	24" X 24"								
30		M1-6	STH 21	24" X 24"								
30		M1-6	STH 22	24" X 24"								
30		M1-6	STH 22	24" X 24"								
30		M6-1	UP ARROW	21" X 21"								
30		M6-1	UP ARROW	21" X 21"								
30		M6-1	RIGHT ARROW	21" X 21"								
30		M6-1	RIGHT ARROW	21" X 21"								
30	3-02	R1-1	STOP	36" X 36"		1	7.46			1	1	
30	3-03		D DOUBLE DOWN ARRO							1	1	REMOVE ONLY
30	3-04	R1-1	STOP	36" X 36"		1	7.46			1	1	
30	3-05	D1-3	THREE DESTINATIONS		2		19.50					
30	3-06	J2-4		96" X 57"	2		38.00					
30		M3-1	NORTH	24" X 12"								
30		M3-2	EAST	24" X 12"								
30		M3-1	NORTH	24" X 12"								
30		M3-3	SOUTH	24" X 12"								
30		M1-6	STH 73	24" X 24"								
30		M1-6	STH 21	24" X 24"								
30		M1-6	STH 22	24" X 24"								
30		M1-6	STH 73	24" X 24"								
30		M6-1	LEFT ARROW	21" X 21"								
30		M6-1	LEFT ARROW	21" X 21"								
30		M6-1	RIGHT ARROW	21" X 21"								
30		M6-1	RIGHT ARROW	21" X 21"								
30	3-07	R4-7	KEEP RIGHT	24" X 30"						1	1	REMOVE ONLY, MOUNTED TO BACK OF 3-02
30	3-08	D1-3	THREE DESTINATIONS							1		MOUNTED TO BACK OF 3-01
				CAT 30 SUBTOTAL	6	2	110.42	0.00	0	6	6	

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

J

SIGN LISTING

CATEGORY	SIGN NO.	SIGN CODE		SIZE	634.0616 POSTS WOOD 4X6-INCH 16-FT EACH	634.0812 POSTS TUBULAR STEEL 2"X2"X12-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2102 MOVING SIGNS TYPE II EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
40	4.04	10.0		70" VEZ"		0	20.50			4	2	
40	4-01	J2-3 M3-1	NORTH	72" X57" 24" X 12"		2	28.50			1	2	
40		M3-1	EAST	24 X 12 24" X 12"								
<u>40</u> 40		M3-3	SOUTH	24 X 12 24" X 12"								
40		M1-6	STH 73	24 X 12 24" X 24"								
40		M1-6	STH 23	24 × 24 24" × 24"								
40		M1-6	STH 73	24 X 24 24" X 24"								·
40		M6-1	LEFT ARROW	24 X 24 21" X 21"								
40		M6-1	RIGHT ARROW	21" X 21"								
40		M6-1	RIGHT ARROW	21" X 21"								
40	4-02	J2-2	MOITI ANNOW	48" X 57"		1	19.00			1	1	
40	7 02	M3-4	WEST	24" X 12"		<u>'</u>				' 	' 	
40		M3-1	NORTH	24" X 12"								
40		M1-6	STH 23	24" X 24"								
40		M1-6	STH 73	24" X 24"								
40		M6-1	LEFT ARROW	21" X 21"								
40		M6-1	UP ARROW	21" X 21"								
40	4-03		ONCOMING TRAFFIC DOES	30" X 12"						1		REMOVE ONLY
			NOT STOP									
40	4-05	R1-1	STOP	36" X 36"		1	7.46			1	1	
40	4-05	R1-1	STOP	36" X 36"		1	7.46			1	1	
40	4-06	W12-1	DOUBLE DOWN ARROW							1	1	REMOVE ONLY
40	4-07	R1-1-R	NO PARKING ANY TIME	18" X 24"		1	3.00			1	1	_
40	4-08	R1-1	STOP	36" X 36"						1	1	REMOVE ONLY
40	4-09	J2-1	KEEP RIGHT	18" X 24"						1	1	REMOVE ONLY
40	4-10	J2-1	WEST 23, LEFT ARROW	24" X 57"						1	1	REMOVE ONLY
		M3-4	WEST									
		M1-6	STH 23									
		M6-1	LEFT ARROW									
			CAT 4	0 SUBTOTAL	0	6	65.42	0	0	10	10	
				TOTALS	7	16	203.80	4.00	7	22	26	

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

CATEGORY	STATION - STATION	650.4000 STORM SEWER SYSTEM EACH	650.4500 SUBGRADE LF	650.5500 CURB & GUTTER LF	650.7000 CONCRETE PAVEMENT LF	650.9920 SLOPE STAKES LF	REMARKS
40	0.007 0.007	0	074	074	074	070	
10	0+22Z - 3+00Z	2	274	274	274	278	IOI AND
10	17+95ND - 18+35ND		40	81			ISLAND
10	194+81N - 195+32N		51	101			MEDIAN ISLAND
	CAT 10 SUBTOTAL	2	365	456	274	278	
20	2+27Y - 3+88Y		144	144	144	161	
20	202+14B - 202+77B		63	150			ISLAND
	CAT 20 SUBTOTAL	0	207	294	144	161	
30	1+07X - 2+92X		180	180	180	185	
30	201+93D - 202+27D		34	87			ISLAND
	CAT 30 SUBTOTAL	0	214	267	180	185	
40	202+41E - 203+38E		147	168	147		TYPE R & TYPE A MODIFIED
40	101+21F - 102+73F		152	152	152		
40	102+34F - 102+78F		44	103			ISLAND

1440

299

897

CONSTRUCTION STAKING

			628.7504	
CATEGORY	STATION	LOCATION	LF	REMARKS
				_
10	0+21Z	LT	15	
10	2+91Z	LT	15	
10	3+52Z	LT	15	
10	20+00ND	LT	15	
10	UNDISTRIBU	JTED	20	
	CAT	10 SUBTOTAL	80	
20	2+09Y	LT	10	
20	3+81Y	LT	10	
20	UNDISTRIBL	JTED	10	
	CAT	20 SUBTOTAL	30	
		-		
30	1+84X	LT	10	
30	2+92X	LT	15	
30	UNDISTRIBL	JTED	10	
		-	• •	

TEMPORARY DITCH CHECKS

FLEXIBLE DELINEATORS

2

343

1129

CAT 40 SUBTOTAL

TOTALS

CATEGORY	STATION-STATION	LOCATION	SPACING FT	633.0200 EACH	REMARKS
10	0+22Z - 2+96Z	LT	50	5	4 FT OFFSET FROM EDGE OF PAVEMENT
		CAT 10	SUBTOTAL =	5	
20	2+38Y - 3+82Y	LT	58	3	4 FT OFFSET FROM EDGE OF PAVEMENT
		CAT 20	SUBTOTAL =	3	
30	1+12X - 2+92X	LT	50	4	4 FT OFFSET FROM EDGE OF PAVEMENT
		CAT 30	SUBTOTAL =	4	
			TOTALS =	12	

ISLAND NOSE AND CURB EPOXY

CAT 30 SUBTOTAL

TOTAL

CATEGORY	STATION - STATION	LOCATION	647.0456 CURB EPOXY LF	647.0606 ISLAND NOSE EPOXY EACH	REMARKS
10	194+81N - 195+23	LT	88	1	MEDIAN, YELLOW
	CA	T 10 SUBTOTAL	88	1	
		TOTAL	88	1	

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: FILE NAME: 1009-44-66 misc_q.ppt PLOT DATE : 2/25/2016 11:30 AM PLOT BY: ZACH GRULING © PLOT NAME : ___ PLOT SCALE: 1:1

SHEET:

PLOT SCALE : 1:1

649.0400

649.0506

		STAGE		643.0300 DRUMS	_	643.0410 TYPE II	BARRIC	CADES 643.0420 TYPE III		643.0705 WARNING LIGHTS TYPE A		643.0715 WARNING LIGHTS TYPE C		643.0900 TRAFFIC CONTROL SIGNS	MARKING F	Y PAVEMENT REMOVABLE 4-INCH YELLOW	TEMPORARY PAVEMENT MARKING REMOVABLE MASK-OUT TAPE 6-INCH	
CATEGORY	LOCATION	DAYS	QTY	DAYS	QTY	DAYS	QTY	DAYS	QTY	DAYS	QTY	DAYS	QTY	DAYS	LF	LF	LF	REMARKS
10	STAGE 1	14	20	280	1	14	0	0	0	0	0	0	15	210				
10	STAGE 2	7	41	287	4	28	2	14	2	14	6	42	20	140	567	711	442	
															567	711	442	_
	CAT 10 SUBTOTAL			567		42		14		14		42		350	12	78	442	=
20	STAGE 1	14	15	210	1	14	0	0	0	0	0	0	29	406				
20	STAGE 2	7	8	56	3	21	0	0	0	0	0	0	25	175	12	225	224	
															12	225	224	_
	CAT 20 SUBTOTAL			266		35		0		0		0		581	2:	37	224	-
30	STAGE 1	14	18	252	0	0	0	0	0	0	0	0	18	252				
30	STAGE 2	7	7	49	3	21	0	0	0	0	0	0	34	238	32	178	166	
															32	178	166	_
	CAT 30 SUBTOTAL			301		21		0		0		0		490	2	10	166	-
40	STAGE 1	7	6	42	6	42	0	0	0	0	0	0	34	238	28	183	182	
40	STAGE 2	10	6	60	0	0	0	0	0	0	0	0	19	190				
40	STAGE 3	14	10	140	0	0	0	0	0	0	0	0	19	266				
															28	183	182	=
	CAT 40 SUBTOTAL			242		42		0		0		0		694	2	11	182	
	TOTALS			1376	= =	140	= =	14	= :	14	= :	42		2115	19	36	1014	⊒

PAVEMENT MARKING EPOXY

	CATEGORY	STATIO	N - STATION	LOCATION	* 4-INCH WHITE LF	* 4-INCH YELLOW LF	646.0106 4-INCH LF	646.0126 8-INCH WHITE LF
	10	192+08N - 1	195+31N	RT	292	323	615	142
	10	17+96ND - 2		RT	140	219	359	
	10	18+25ND - 1	18+61ND	LT				70
	10	193+64N - 1	194+55N	LT	92			
				CAT 10 SUBTOTAL			974	212
	20	202+67B - 2	203+17B	LT				100
				CAT 20 SUBTOTAL			0	100
	30	201+86D - 2	202+04	RT				36
				CAT 30 SUBTOTAL			0	36
	40	102+10F - 1	02+47F	RT				73
				CAT 40 SUBTOTAL			0	73
	* FOR INFORM	MATION ONLY		TOTAL			974	421
HWY: VA	RIOUS		COUNTY: VAI	RIOUS	MISC	ELLANEOUS	QUANTITIES	

FILE NAME : 1009-44-66 misc_q.ppt PLOT DATE : 2/25/2016 11:30 AM PLOT BY : ZACH GRULING © PLOT NAME : ______

PROJECT NO: 1009-44-66

PULL BOXES, ADJUSTING & REMOVING PULL BOXES

CATEGORY	STATION	LOCATION	652.0225 CONDUIT RIGID NONMETALLIC SCH 40 2-INCH LF	653.0110 PULL BOXES STEEL 12X36-INCH EACH	653.0900 ADJUSTING PULLBOXES EACH	653.0905 REMOVING PULLBOXES EACH	SPV.0090.02 FURNISH AND INSTALL EQUIVALENT LIGHTING CONDUCTORS LF	REMARKS
20	202+22B	LT			1			IN ISLAND
20	202+46B	LT			<u></u>	1		CONDUIT TO REMAIN
	C	AT 20 SUBTOTAL			1	1		
30	202+20D	RT				1		CONDUIT TO REMAIN
	C	AT 30 SUBTOTAL			0	1		
40	202+18E	LT			1			
40	202+41E	LT		1				
40	202+38E - 203+38	E LT	100				400	
40	203+38E	LT		1				
40	203+38E - 206+2	3 LT					1140	
40	102+62F	LT			1			
40	102+66F	RT			1			IN ISLAND
	C	AT 40 SUBTOTAL	100	2	3	0	1540	
		TOTAL	100	2	4	2	1540	

PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

FILE NAME : 1009-44-66 misc_q.ppt PLOT BY : ZACH GRULING PLOT NAME : _____ PLOT SCALE : 1:1

3

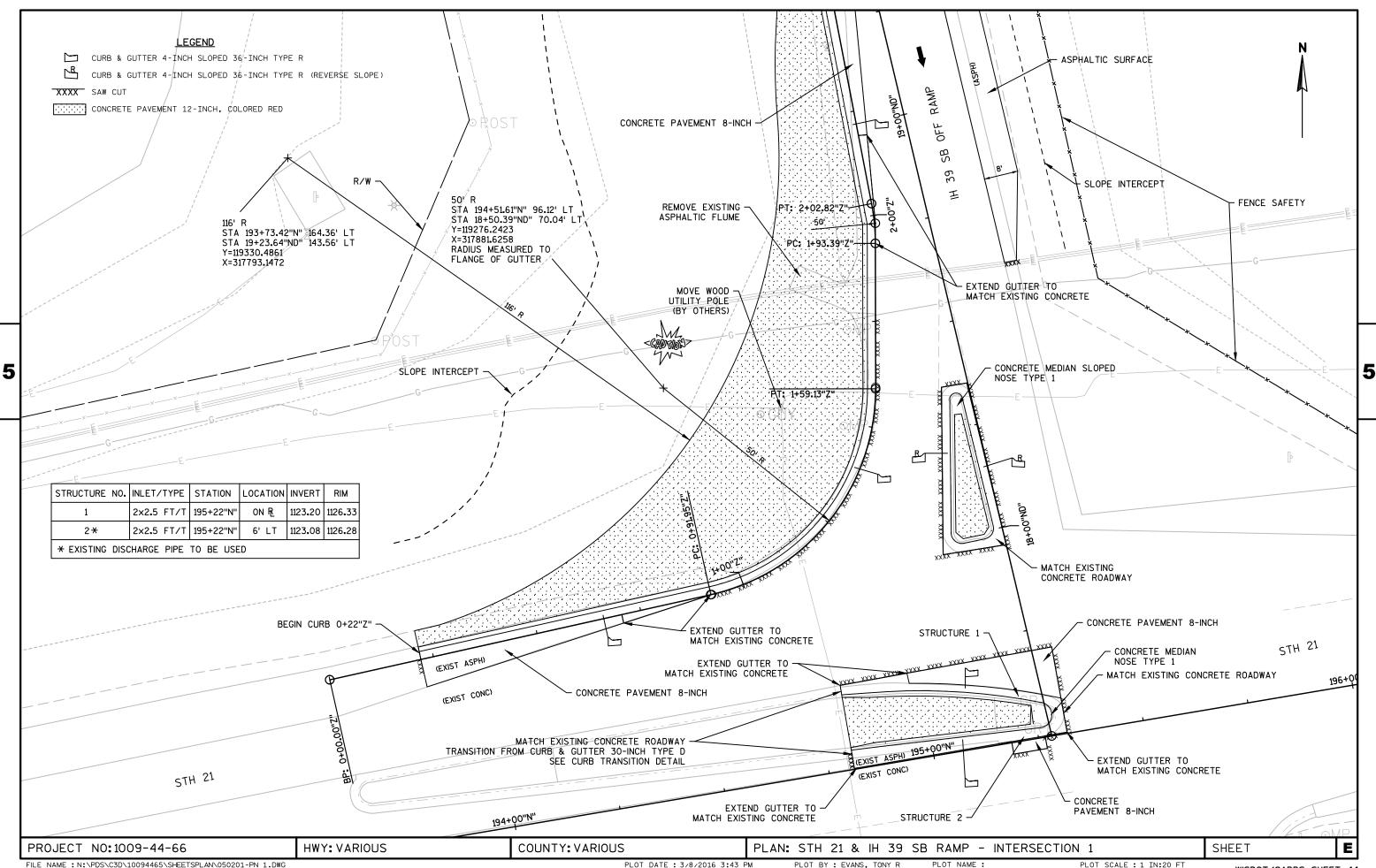
SAWING

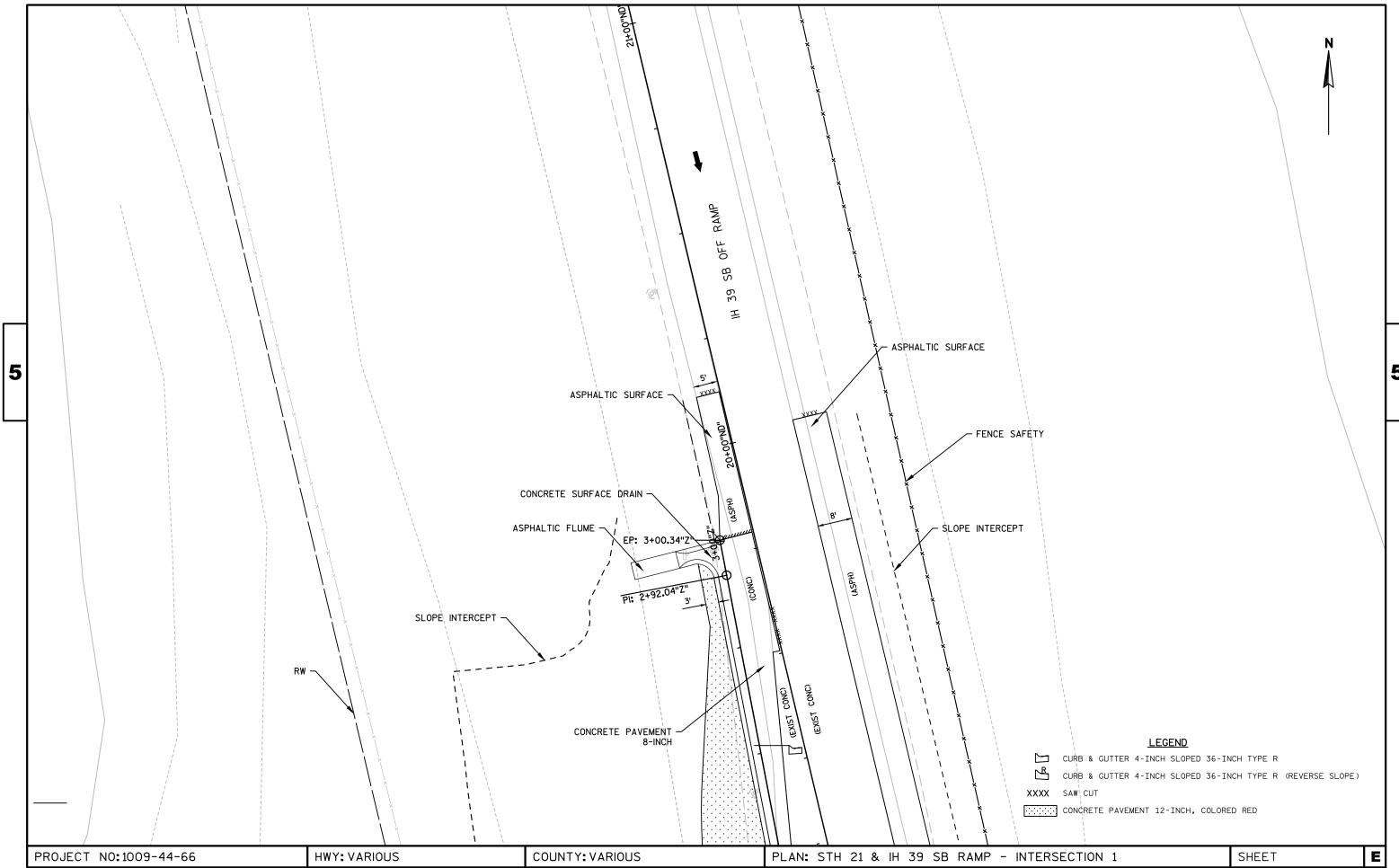
			690.0150	690.0250	
			SAWING ASPHALT	SAWING CONCRETE	
CATEGORY	STATION	LOCATION	LF	LF	REMARKS
OATEOORT	OTATION	LOOKHON	<u> </u>		I LIVIAI (I C
10	0+22Z	RT	10		
10	0+92Z - 1+76Z	NW RADIUS		84	
10	20+12ND	LT	5		
10	20+02ND	RT	3		
10	18+60ND	RT	3		
10	17+95ND - 18+35ND	ISLAND		100	
10	194+81N - 195+32N	MEDIAN ISLAND		96	
10	194+81N	MEDIAN ISLAND	3	3	CURB & GUTTER
		CAT 10 SUBTOTAL	24	283	
20	2+27Y - 3+87Y	NW RADIUS	164		
20	202+14B - 202+77B	ISLAND	167		
		CAT 20 SUBTOTAL	331	0	
	4 4014 0 0014	0144 5 4 5 11 10	400		
30	1+10X - 2+92X	SW RADIUS	192		OUDD A QUITTED
30	2+92X	LT		3	CURB & GUTTER
30	201+93D - 202+27D	ISLAND	118		
		CAT 30 SUBTOTAL	310	3	
40	202+41E - 203+38E	RT	100		
40	202+41E	LT		3	CURB & GUTTER
40	202+88E	LT		3	SIDEWALK
40	203+22E	LT		16	DRIVEWAY APRON
40	203+38E	LT		3	CURB & GUTTER
40	101+21F - 102+73F	RT	154		
40	101+21F	LT		3	CURB & GUTTER
40	101+41F	LT		16	DRIVEWAY APRON
40	102+12F	LT		12	DRIVEWAY APRON
40	102+73F	LT		3	CURB & GUTTER
40	102+34F - 102+78F	ISLAND	135		
		CAT 40 SUBTOTAL	389	59	
		TOTAL	1054	345	

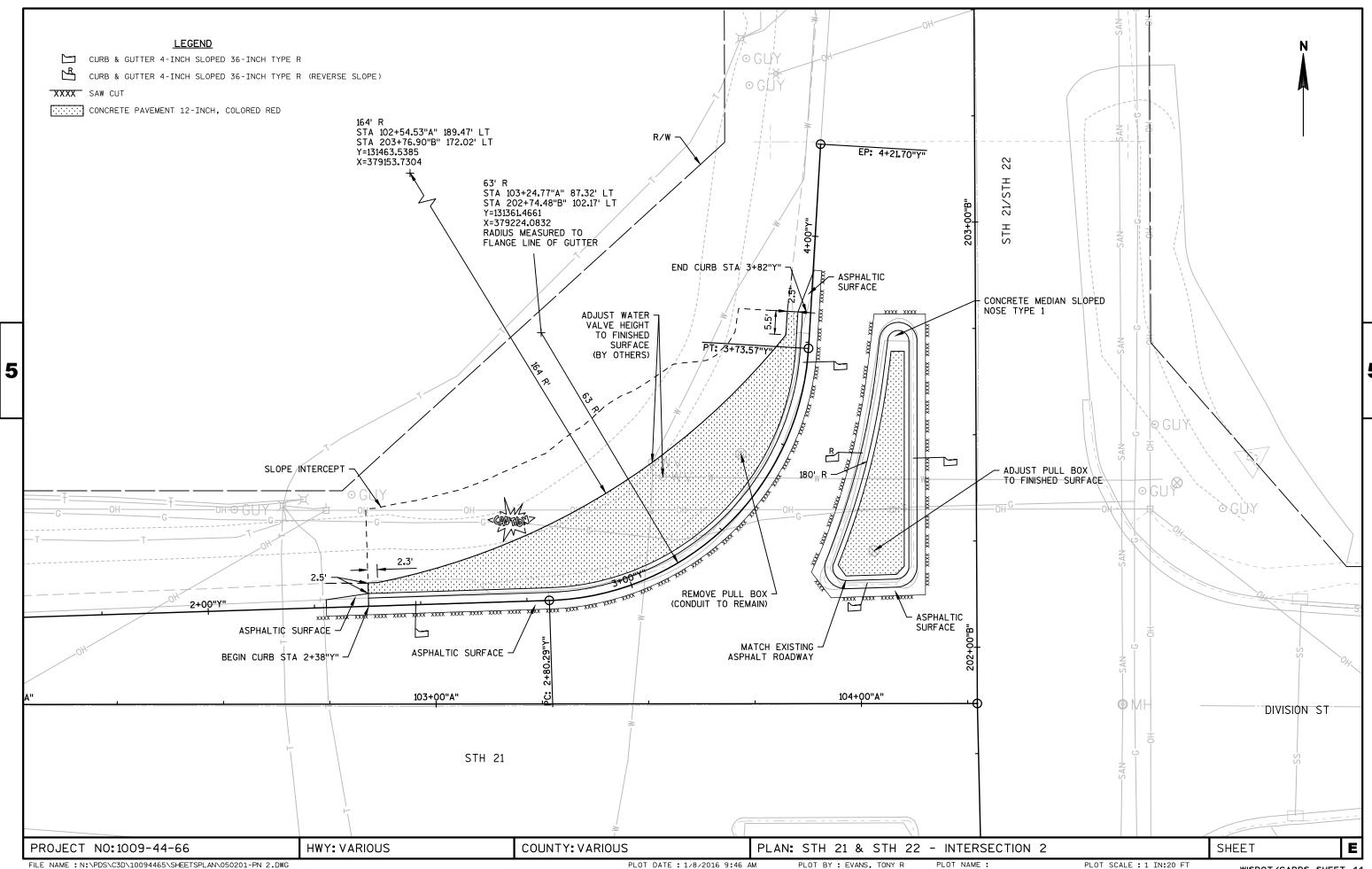
CONCRETE PAVEMENT JOINT LAYOUT

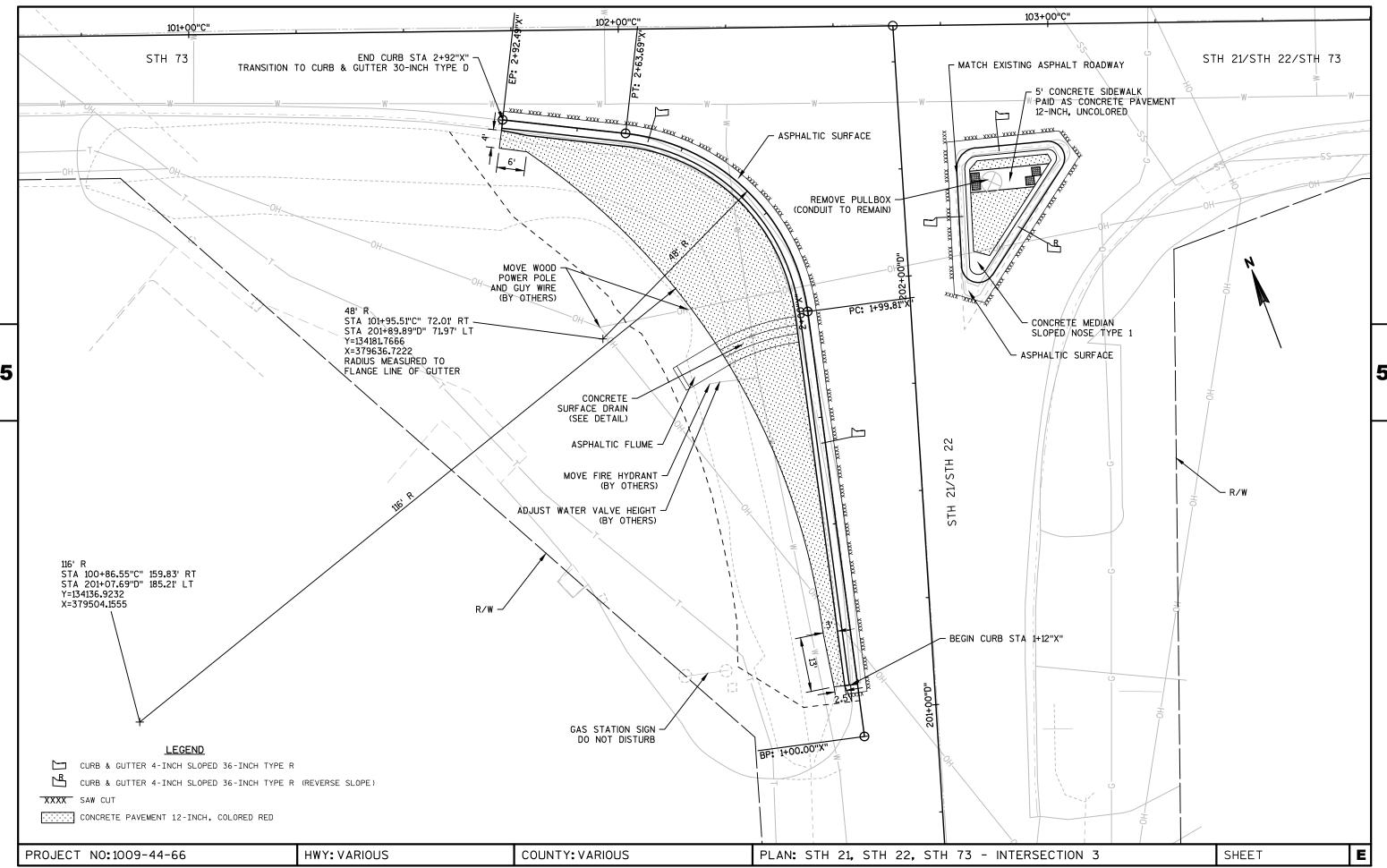
CATEGORY	LOCATION	SPV.0105 LS
10	STH 21 & IH 39 SB OFF RAMP - INTERSECTION 1	0.25
	CAT 10 SUBTOTAL	0.25
20	STH 21 & STH 22 - INTERSECTION 2	0.25
	CAT 20 SUBTOTAL	0.25
30	STH 21, STH 22 & STH 73 - INTERSECTION 3	0.25
	CAT 30 SUBTOTAL	0.25
40	STH 23 & STH 73 - INTERSECTION 4	0.25
	CAT 40 SUBTOTAL	0.25
	TOTAL	1.0

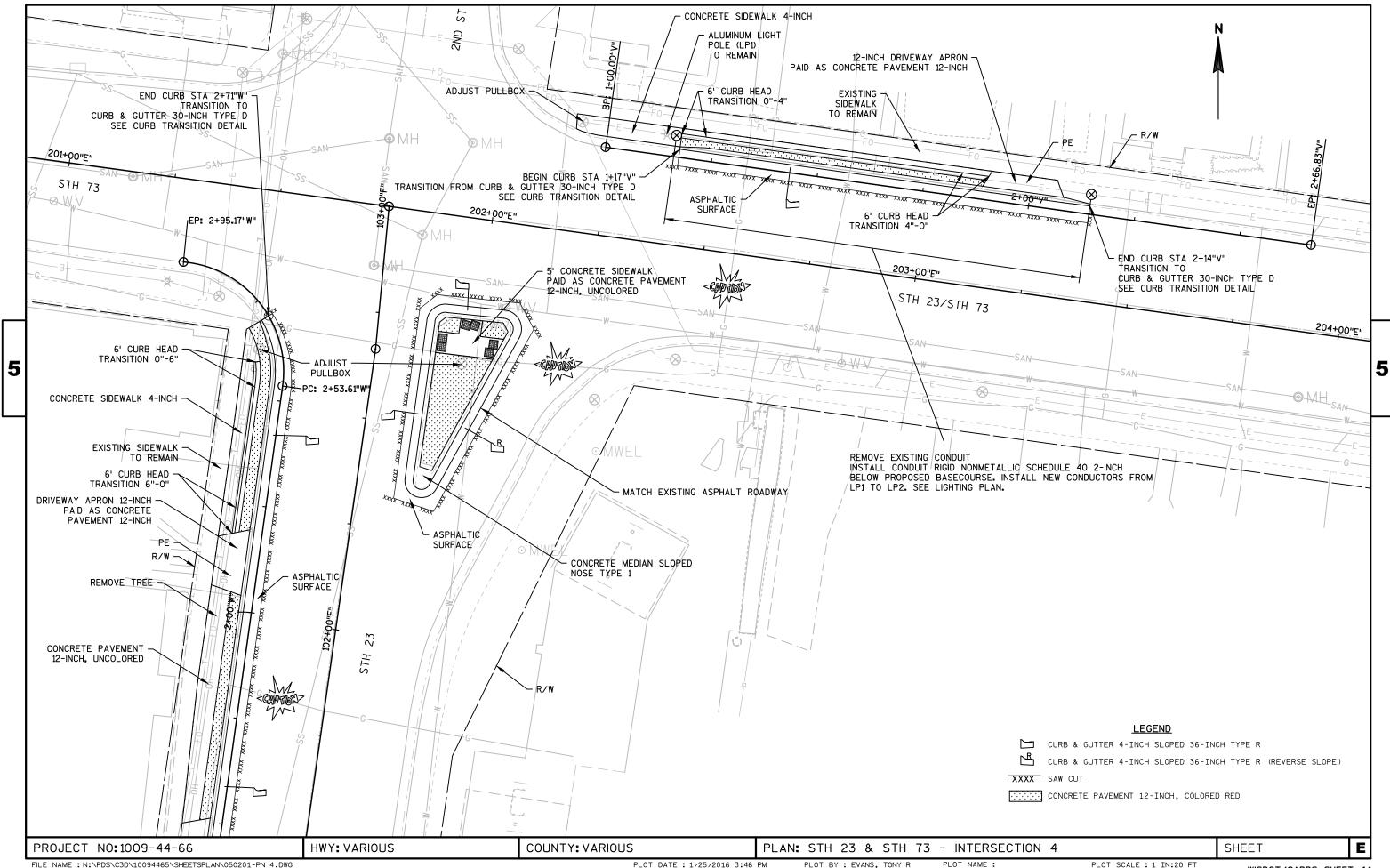
PROJECT NO: 1009-44-66 HWY: VARIOUS COUNTY: VARIOUS MISCELLANEOUS QUANTITIES SHEET: **E**

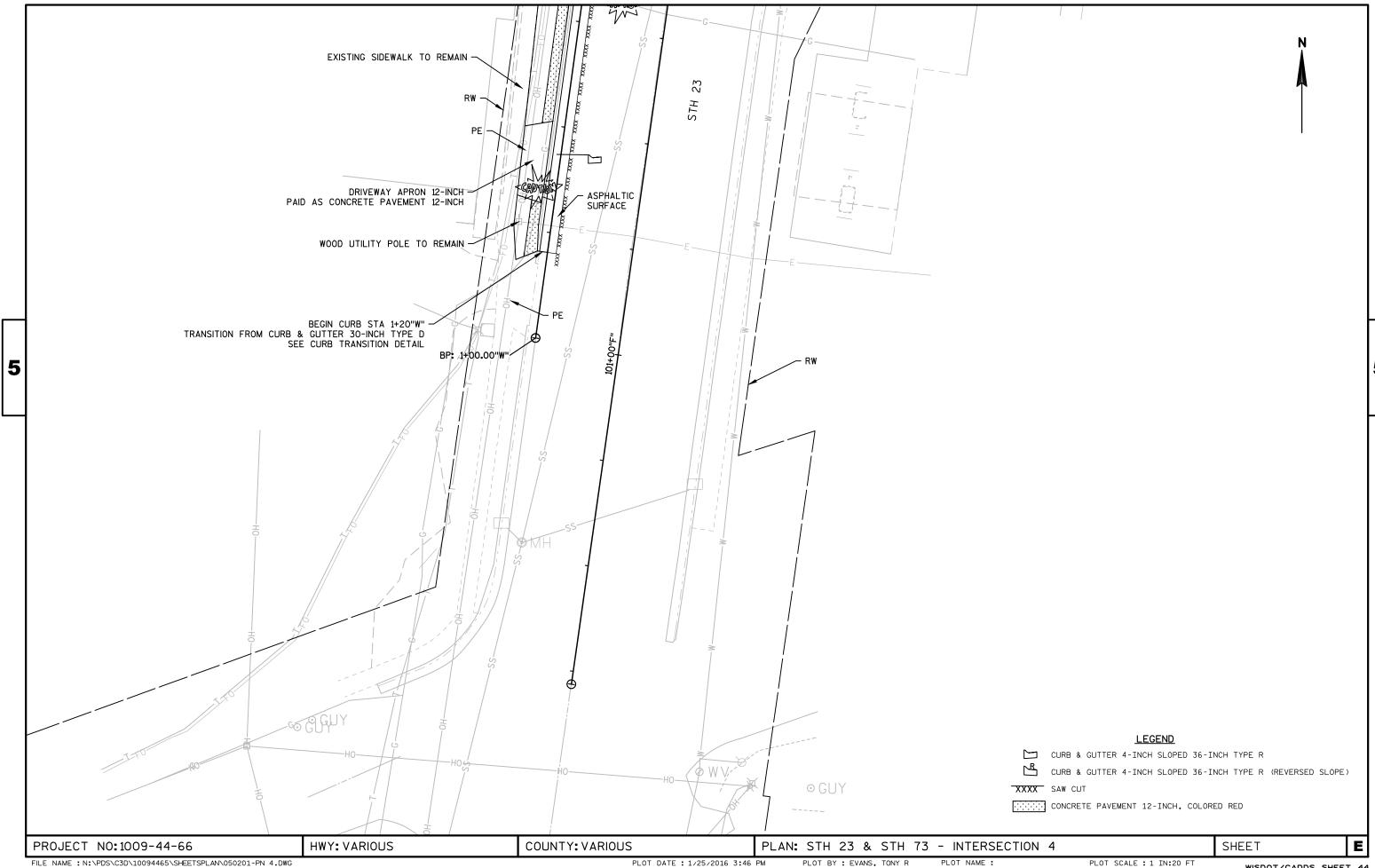






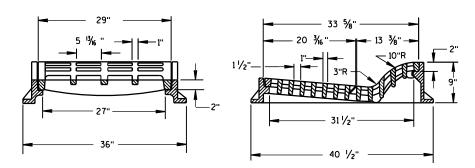






Standard Detail Drawing List

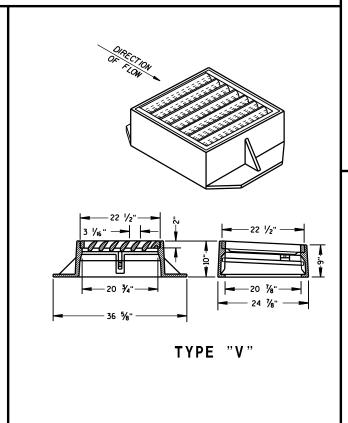
08A05-19C	INLET COVERS TYPE F, HM, HM-S, S, T, V, HM-GJ, & HM-GJ-S
08C07-01	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08D01-18	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D04-05	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-16A	CURB RAMPS TYPES 1 AND 1-A
08D05-16B	CURB RAMPS TYPES 2 AND 3
08D05-16C	CURB RAMPS TYPES 4A AND 4A1
08D05-16D	CURB RAMPS TYPE 4B AND 4B1
08D05-16E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09B02-09	CONDUI T
09B04-11	PULL BOX
11B02-02	CONCRETE MEDIAN NOSE
13C01-18	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C11-11A	RURAL DOWELED CONCRETE PAVEMENT
13C11-11B	RURAL DOWELED CONCRETE PAVEMENT
13C18-03A	CONCRETE PAVEMENT JOINTING
13C18-03B	CONCRETE PAVEMENT STEEL REINFORCEMENT
13C18-03C	CONCRETE PAVEMENT JOINT TIES
13C18-03D	CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES
15A04-02	FLEXI BLE DELI NEATOR POST
15C04-03	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-03	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16F	PAVEMENT MARKING (ISLANDS)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C27-01	DOUBLE ARROW WARNING SIGN PLACEMENT
15D28-03	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-02A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-02C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



TYPE "F"

USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

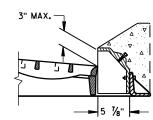
25 ½" 23 ½" 23 ½" 23 ½" 23 ½" 23 ½" 23 ½" 23 ½" 23 ½" 25 ½" 23 ½" 25 ½" 25 ½" 26 ½" 27 ½" 28 ½" 28 ½" 29 ½" 20 ½"



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

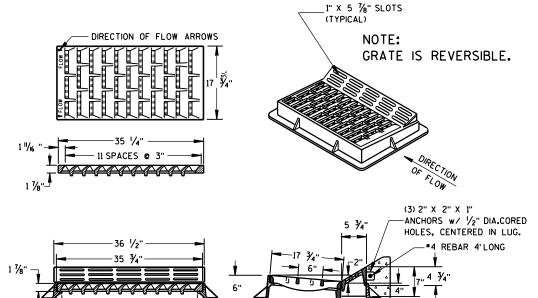
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.



ALTERNATIVE CURB BOX FOR TYPE "HM" COVER

USE WITH TYPES G & J CONCRETE CURB & GUTTER, 30 INCH NOTED AS TYPE HM-GJ ON DRAINAGE TABLE

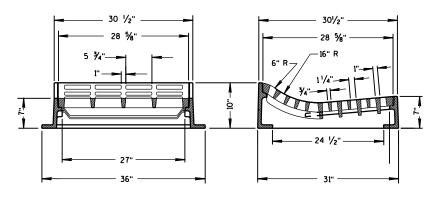
NOIE:
SPECIAL GRATE FOR THE
TYPE "H" COVER MAY ALSO BE
USED FOR THE TYPE "HM-GJ" COVER
NOTED AS TYPE HM-GJ-S ON DRAINAGE TABLE



TYPE "HM"

USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

NOTE:
SPECIAL GRATE FOR THE
TYPE "H" COVER MAY ALSO BE
USED FOR THE TYPE "HM" COVER
NOTED AS TYPE HM-S ON DRAINAGE TABLE



TYPE "T"

USE WITH TYPES R & T CONCRETE CURB & GUTTER, 36 INCH.



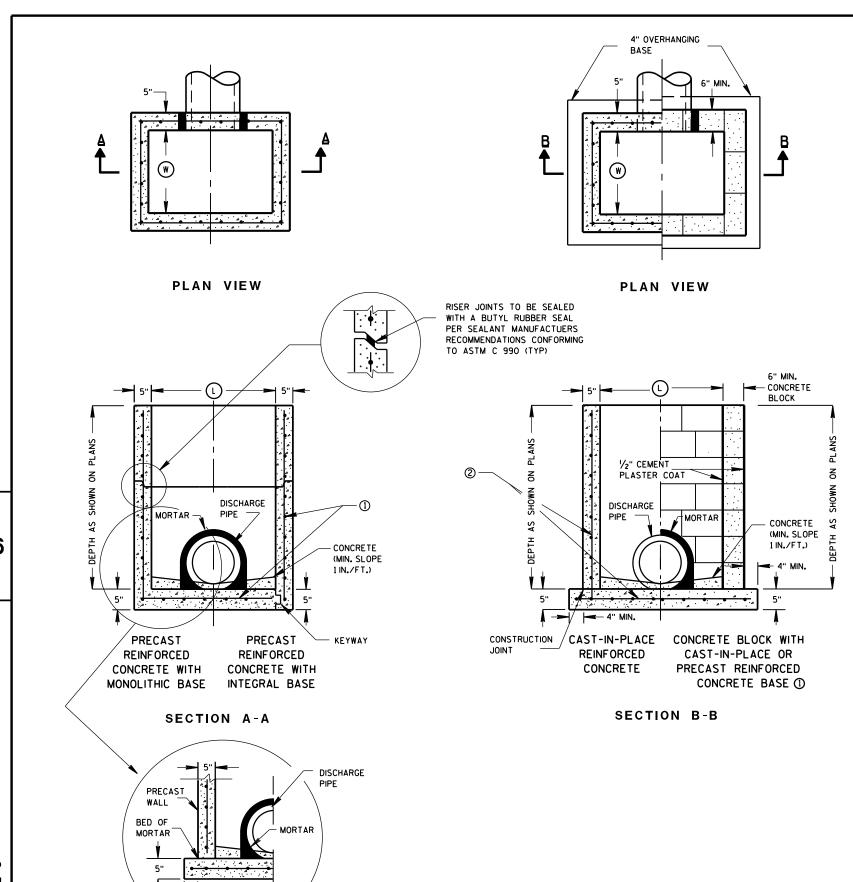
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

II/27/2013
DATE / /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER

A 5-19

D.D. 8



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

- 4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
- 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
- OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

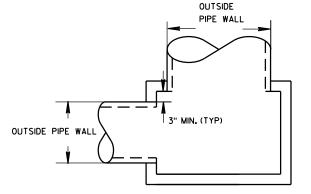
- 1) FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- (2) CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

	INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	s	т	v	WM
		WIDTH (W) (FT)	LENGTH (L) (FT)									
	2X2-FT	2	2	X	х				Х		Х	
ſ	2X2.5-FT	2	2.5			Х			Х	Х	Х	Х
[2X3-FT	2	3					Х				
	2.5X3-FT	2.5	3				Х					

PIPE MATRIX

	MAXIMUM INSIDE PIPE DIAMETER						
INLET SIZE	WIDTH (IN)	LENGTH (IN)					
2X2-FT	12	12					
2X2.5-FT	12	18					
2X3-FT	12	24					
2.5X3-FT	18	24					



DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED 6/5/2012 DATE

FHWA

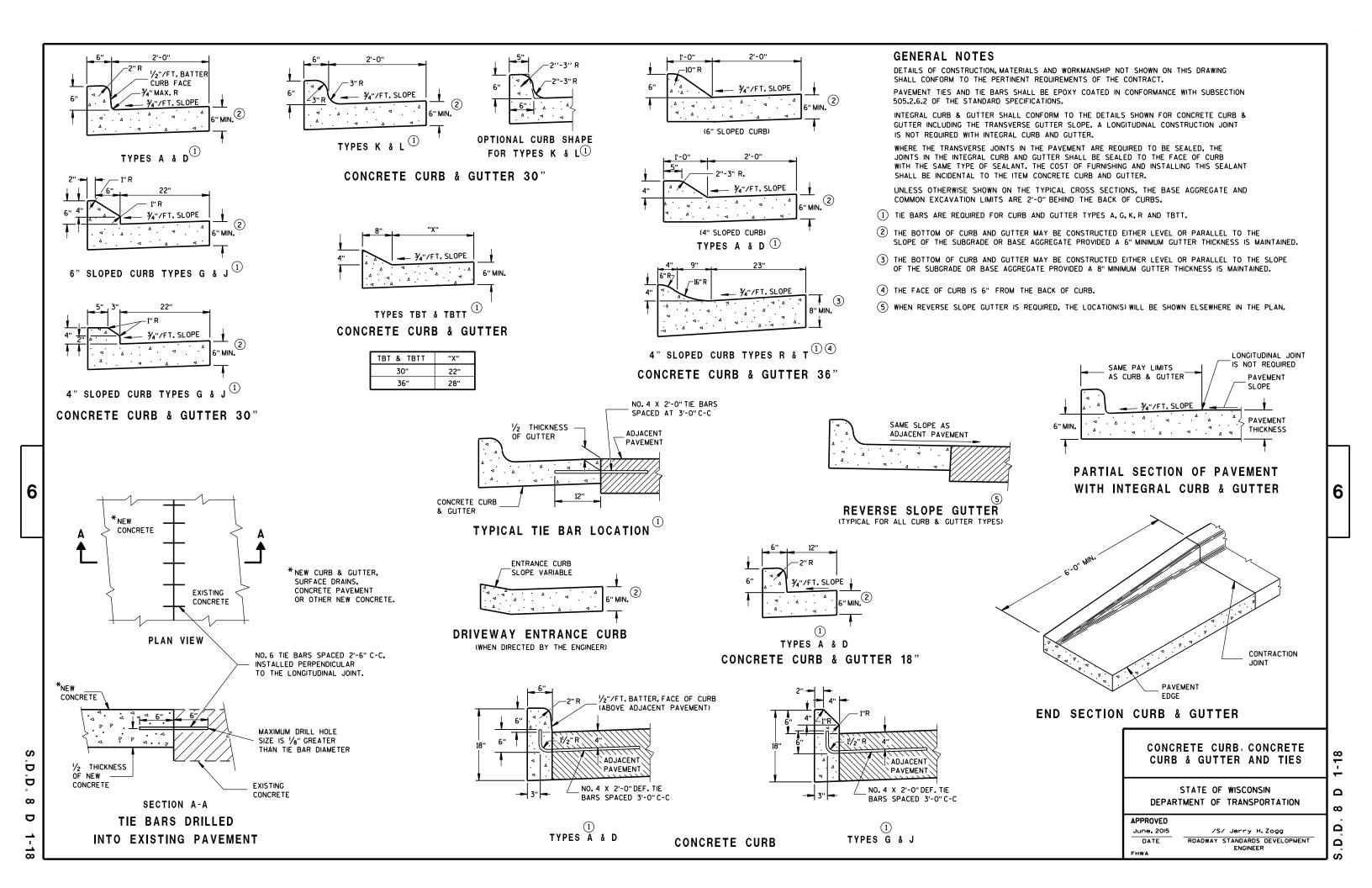
/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT

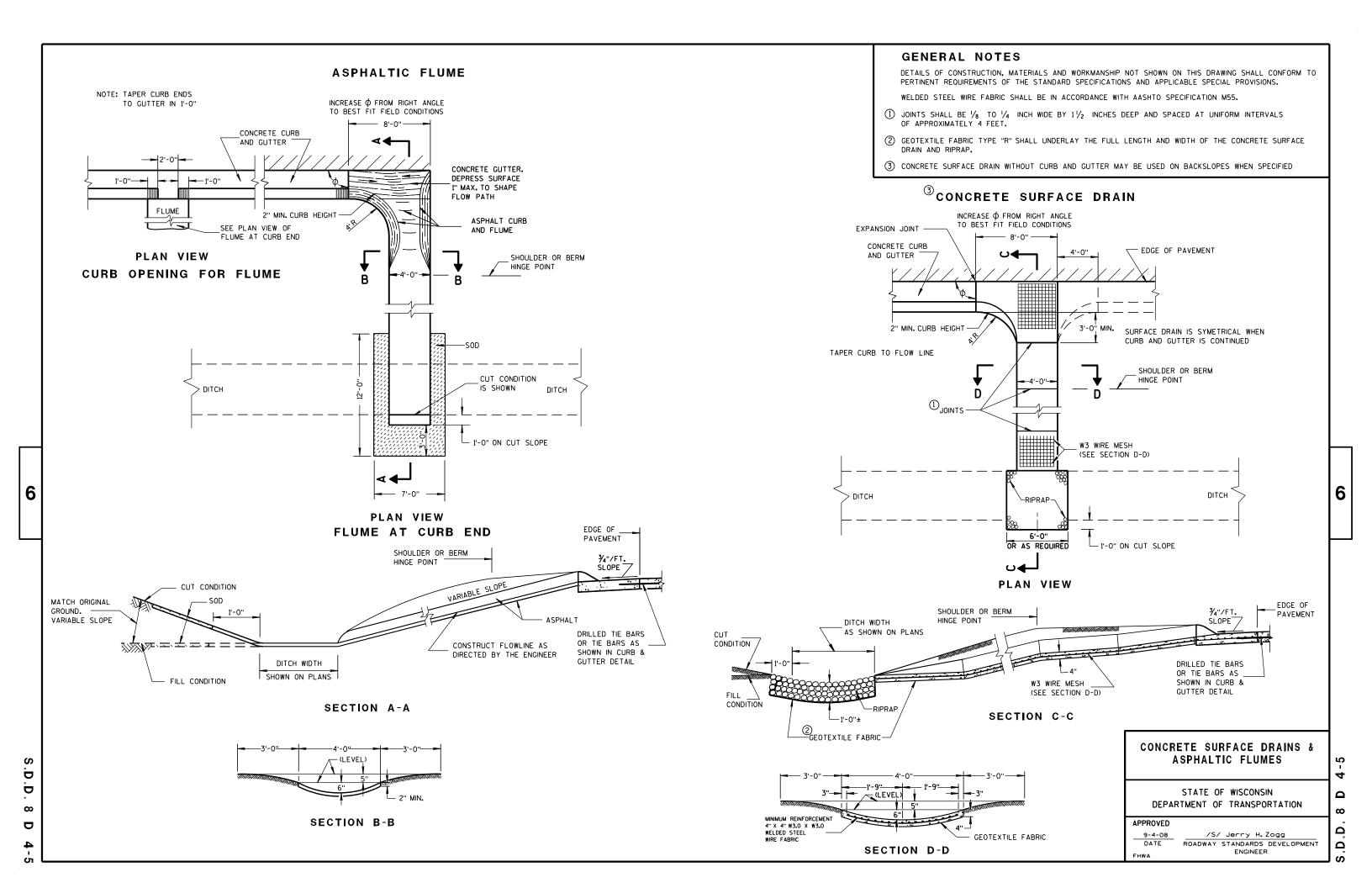
ENGINEER

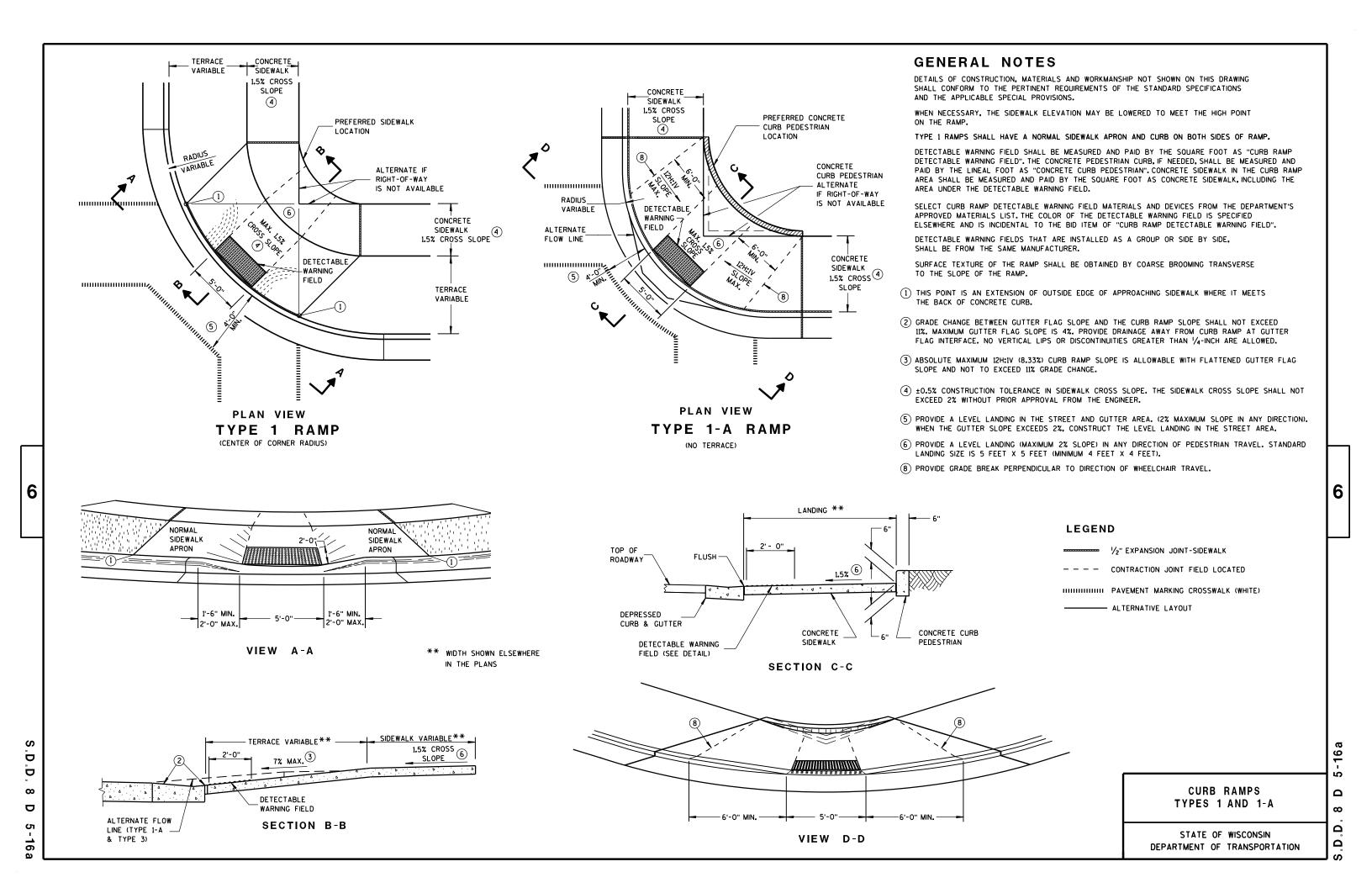
INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

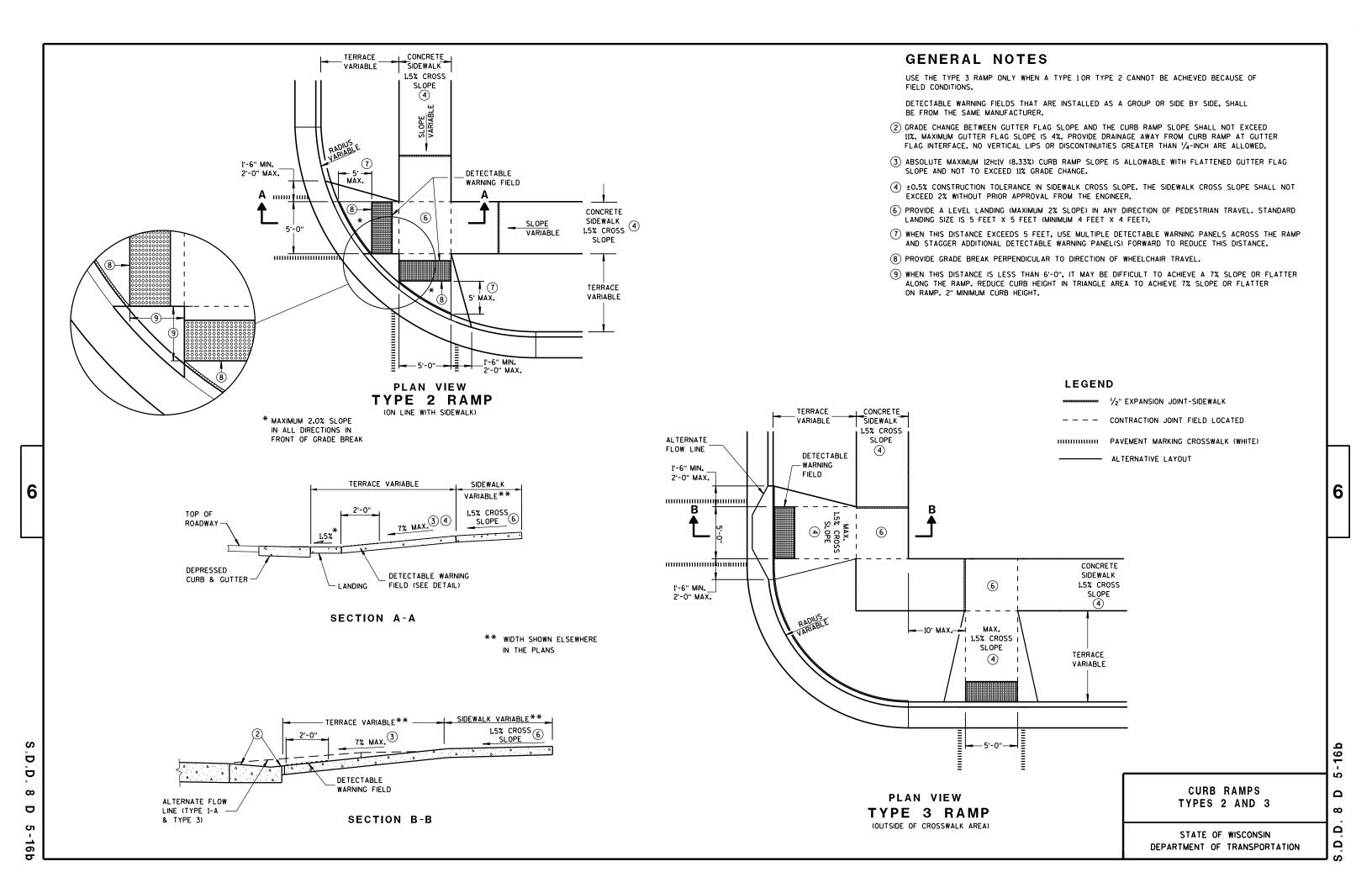
SEPARATE PRECAST REINFORCED

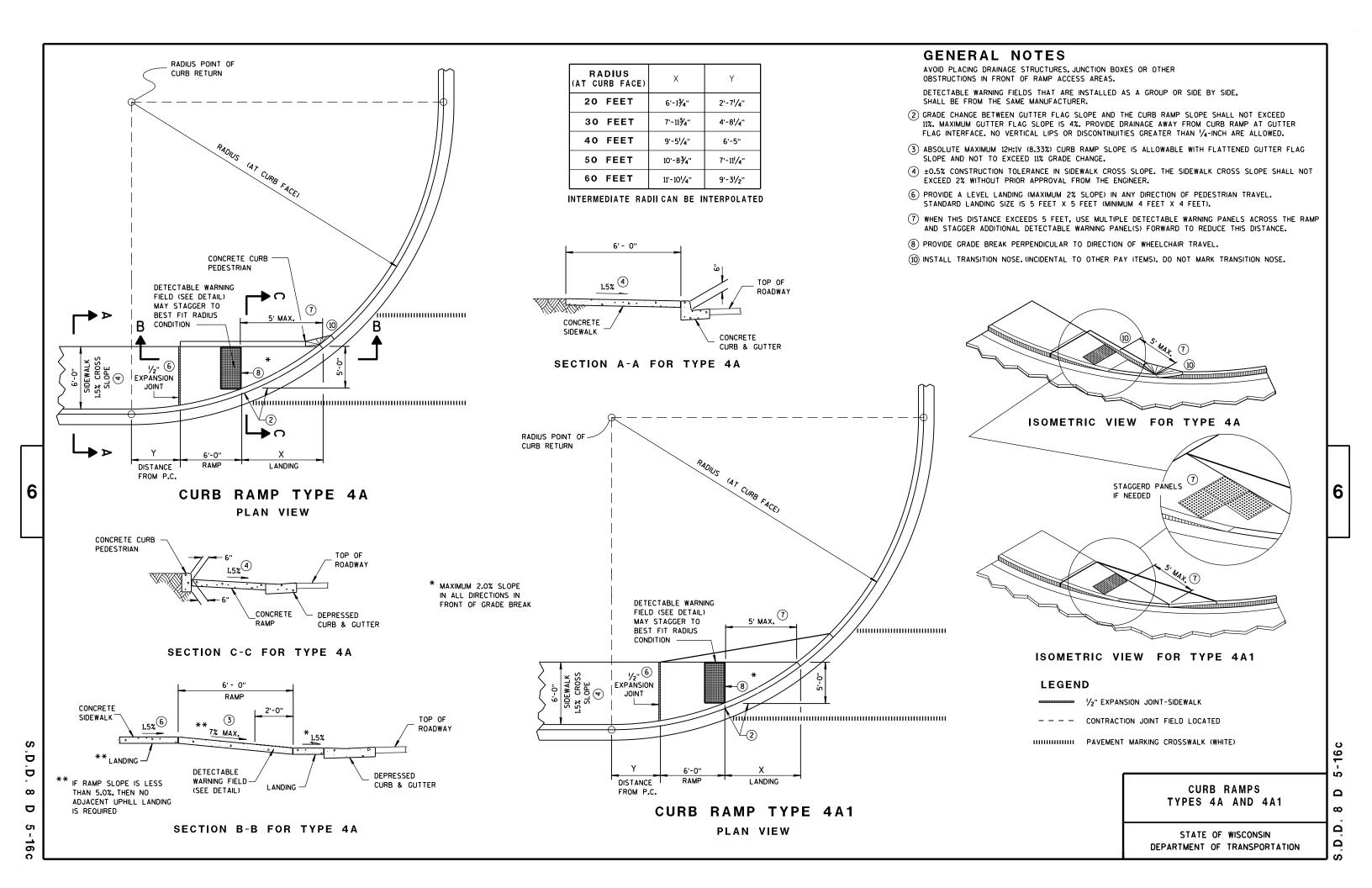
CONCRETE BASE OPTION

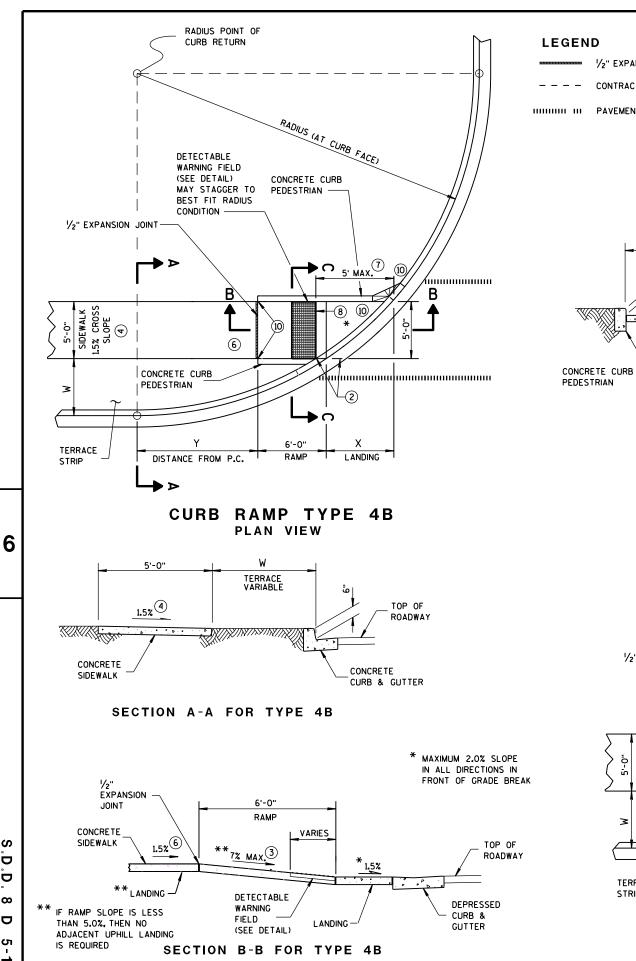












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W = 5' - 0" 7' - Ø" 3' - Ø" W = 4' - Ø" W = 6' - 0"RADIUS LEGEND AT CURB FACE ■ ½" EXPANSION JOINT-SIDEWALK 20 FEET 4'-81/2" 3'-7" 3'-11/2" 4'-61/2" 4'-1" 7'-23/4" 8'-31/2" 9'-21/2" 5'-51/2" 6'-0" CONTRACTION JOINT FIELD LOCATED 30 FEET 6'-51/2" 5'-91/4" 5'-21/2" 4'-8¾" 7'-31/4' 8'-11'/2" 10'-7" 12'-0" 13'-31/4" HIHHHH HI PAVEMENT MARKING CROSSWALK (WHITE) 40 FEET 8'-91/2" 9'-21/2" 11'-5'/4" 13'-41/2" 15'-3/4" 16'-71/4" 50 FEET 7'-61/2" 6'-11¾" 19'-6'/4" 11'-3/4" 15'-91/2"

10'-¾"

GENERAL NOTES

12'-8¾"

11'-2'/2"

60 FEET

TOP OF

ROADWAY

TERRACE STRIP

VARIES O TO W

CONCRETE

CURB & GUTTER

5'-0" RAMP

VARIES

0 TO 6"

1.5%

SECTION C-C FOR TYPE 4B

INTERMEDIATE RADII CAN BE INTERPOLATED

7'-101/2"

22'-11/2"

20'-1¾"

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS. DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

17'-113⁄4"

8'-5¾"

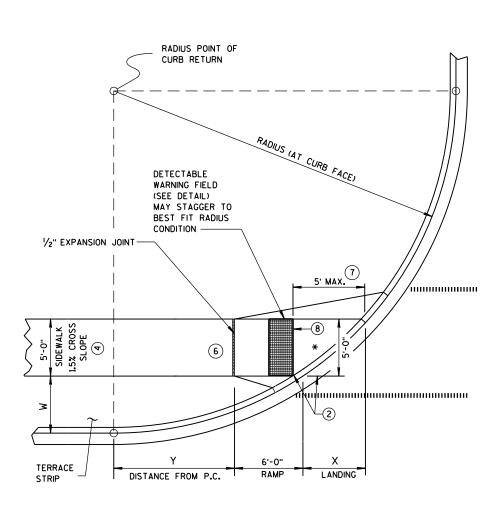
(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4-INCH ARE ALLOWED.

9'-21/4"

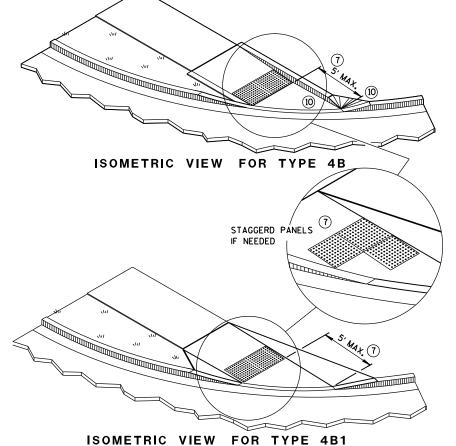
- (3) ABSOLUTE MAXIMUM 12H:1V (8.33%) CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE, THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET X 5 FEET (MINIMUM 4 FEET X 4 FEET).

15'-61/2"

- (7) WHEN THIS DISTANCE EXCEEDS 5 FEET, USE MULTIPLE DETECTABLE WARNING PANELS ACROSS THE RAMP AND STAGGER ADDITIONAL DETECTABLE WARNING PANEL(S) FORWARD TO REDUCE THIS DISTANCE.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (I) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



CURB RAMP TYPE 4B1 PLAN VIEW

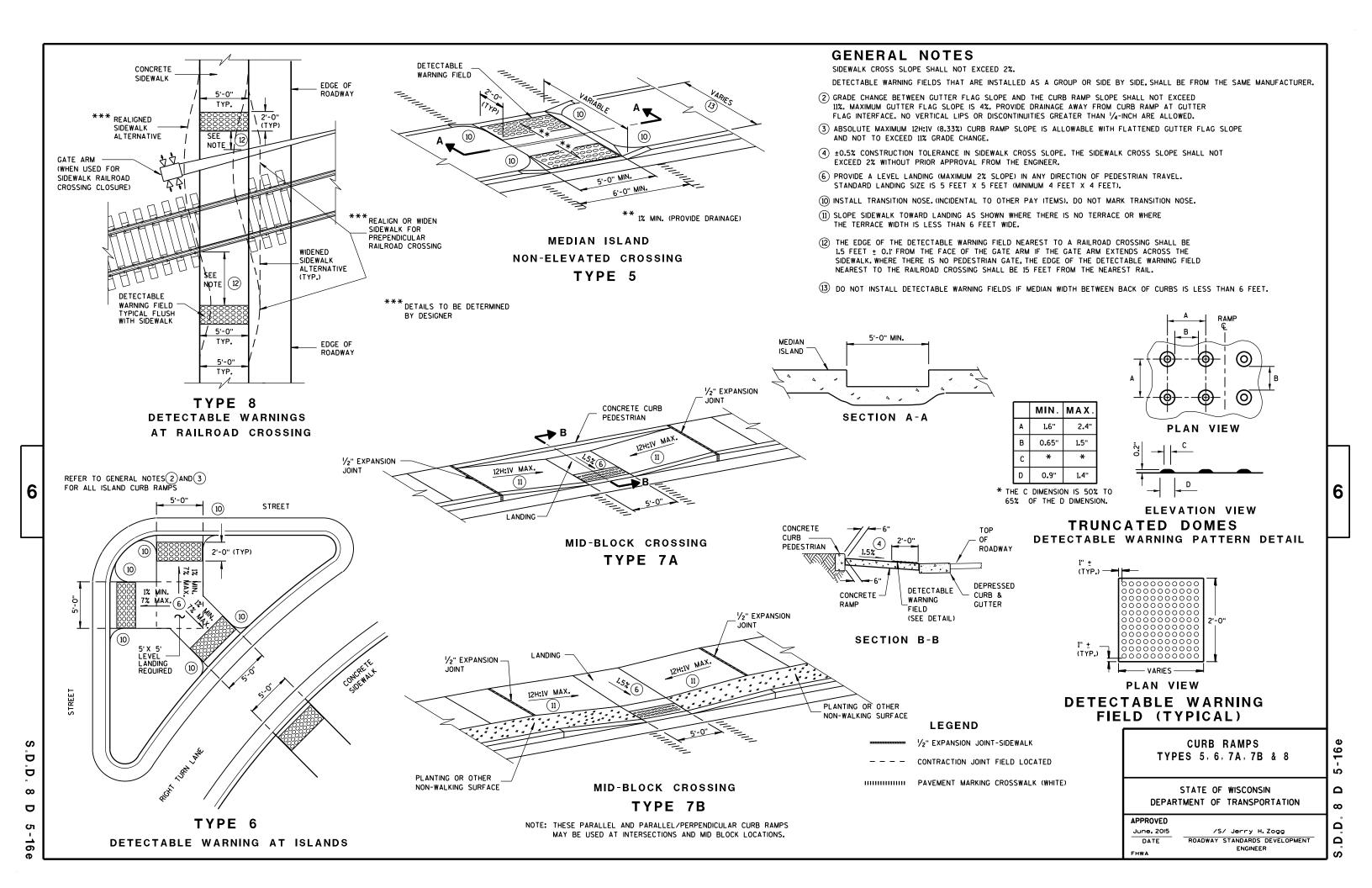


CURB RAMPS TYPE 4B AND 4B1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF **EROSION BALES / TEMPORARY** DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 /S/ Beth Connestro
CHIEF ROADWAY DEVELOPMENT ENGINEER

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INLET PROTECTION, TYPE A

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- 1) FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- (2) FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- (3) FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE, THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION TYPE A, B, C, AND D

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

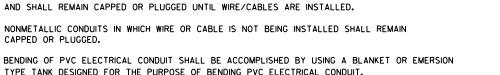
10/16/02

/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER 6

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TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES

SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REIN-STALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY

GENERAL NOTES

AND 36 INCHES MAXIMUM.

OF THE ENGINEER.

CAPPED OR PLUGGED.

MINIMUM AND 36 INCHES MAXIMUM.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

BOTTOM OF ¼" HOLE PVC CONDUIT-CONDUIT TRENCH FOR DRAINAGE NO. 2 COARSE AGGREGATE FILL 1'-0" DIA. OR SQUARE →

NOTE: INSTALL AT LOCATIONS WHERE METALLIC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

BOTTOM OF

CONDUIT TRENCH

NOTE: INSTALL AT LOCATIONS WHERE PVC CONDUITS CANNOT BE PITCHED TO DRAIN INTO A PULL BOX.

DRAIN SUMP FOR METALLIC CONDUIT

1'-0" DIA. OR SQUARE ──➤

METALLIC CONDUIT-

1" DIA. X 6"

NIPPLE

NO. 2 COARSE

AGGREGATE FILL

ARROW MARK SHALL BE INSCRIBED IN PAVEMENT SURFACE 1/4" TO 3/8"

DEEP AT EACH LOCATION WHERE CONDUITS ARE PLACED UNDER

PLAN VIEW

ARROW MARK

CONDUIT

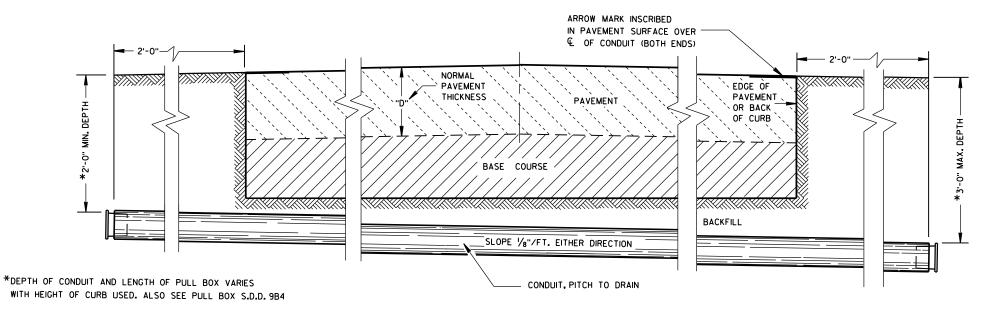
THE PAVEMENT

EDGE OF

PAVEMENT OR BACK

OF CURB

DRAIN SUMP FOR PVC CONDUIT



SIDE ELEVATION DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

CONDUIT

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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APPROVED /S/ Ahmet Demirbilek June. 2015 DATE STATE ELECTRICAL ENGINEER

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DIMENSION IN INCHES				COF	RRUGAT	ED ST	EEL P	IPE		
PIPE DIAMETER (INSIDE)	Α	12	12	12	18	18	18	24	24	24
PIPE LENGTH **	В	24	30	36	24	30	36	36	42	48
WALL THICKNESS	С	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 1/4	10 1/4	10 1/4	16 1/4	16 1/4	16 1/4	22 1/4	22 1/4	22 1/4
FRAME	Ε	14 1/2	14 1/2	14 1/2	20 ½	20 ½	20 ½	26 ½	26 ½	26 ½
FRAME	F	8 1/2	8 1/2	8 1/2	14 1/2	14 ½	14 1/2	20 ½	20 ½	20 ½
FRAME	G	11 1/2	11 1/2	11 1/2	17 1/2	17 1/2	17 1/2	23 ½	23 ½	23 ½
					WEIGH	T IN P	OUNDS	*		
FRAME AND COVER		60	60	60	110	110	110	155	155	155

- * THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.
- NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.

6" MAX. **EXTENSION** TOP OF ORIGINAL CORRUGATED PIPE (3) BOLTS, NUTS & LOCKWASHERS REQUIRED

ELECTRIC

FINAL GRADE

ALL METALLIC CONDUIT

AND THREADED

CUT OPENINGS

THE FIELD

2" PVC PIPE CAP ON BOTH ENDS

WITH 7, 8 1/4" HOLES DRILLED

IN EACH END.

PULL BOX

AS REQUIRED IN

ENDS SHALL BE REAMED

ALL CONDUIT PITCHED

4 TO 8 BRICKS

EQUALLY SPACED

TO DRAIN TO PULL BOXES

2" DRAIN DUCT TO

DITCH OR SEWER

WHEN SPECIFIED

CORRUGATED PIPE EXTENDER

HEAVY DUTY FRAME -

6" MIN.

(TYP.)

AND COVER

WHEN A PULL BOX IS INSTALLED IN CRUSHED

AGGREGATE SHOULDERS, PLACE IT 2-3

2-3 INCHES OF CRUSHED AGGREGATE

NO. 2 COARSE

(SEE SECTION 501

OF THE STANDARD

WIRE AND/OR CABLE.

INSTALL END BELLS (U.L. LISTED FOR

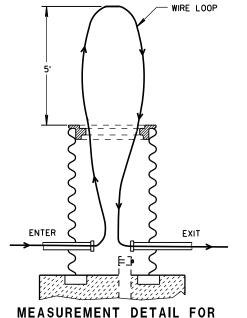
CONDUIT BEFORE INSTALLATION OF

ELECTRICAL USE) ON ALL NONMETALLIC

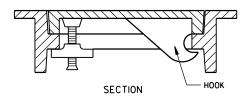
SPECIFICATIONS)

AGGREGATE

INCHES BELOW GRADE AND COVER IT WITH

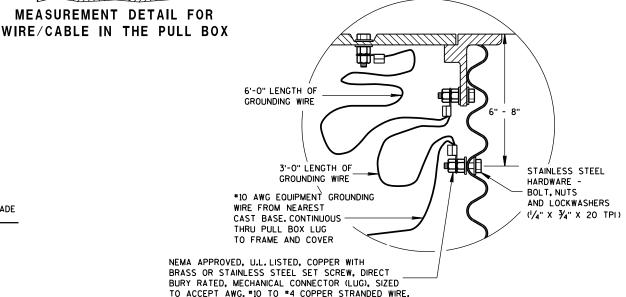


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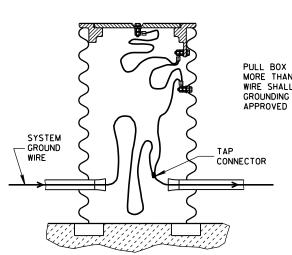


ALTERNATE COVER (LOCKING)

TIGHTENING BAR TYPE



EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES



EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES

PULL BOX TO NEAREST BASE DISTANCE MORE THAN 20 FEET. PULL BOX GROUND WIRE SHALL CONNECT AT SYSTEM GROUNDING WIRE. USE DEPARTMENT APPROVED TAP CONNECTOR.

PULL BOX

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

Sept. 2014 /S/ Ahmet Demirbilek DATE STATE ELECTRICAL ENGINEER FHWA

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED. SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.

TRAFFIC LOADS.

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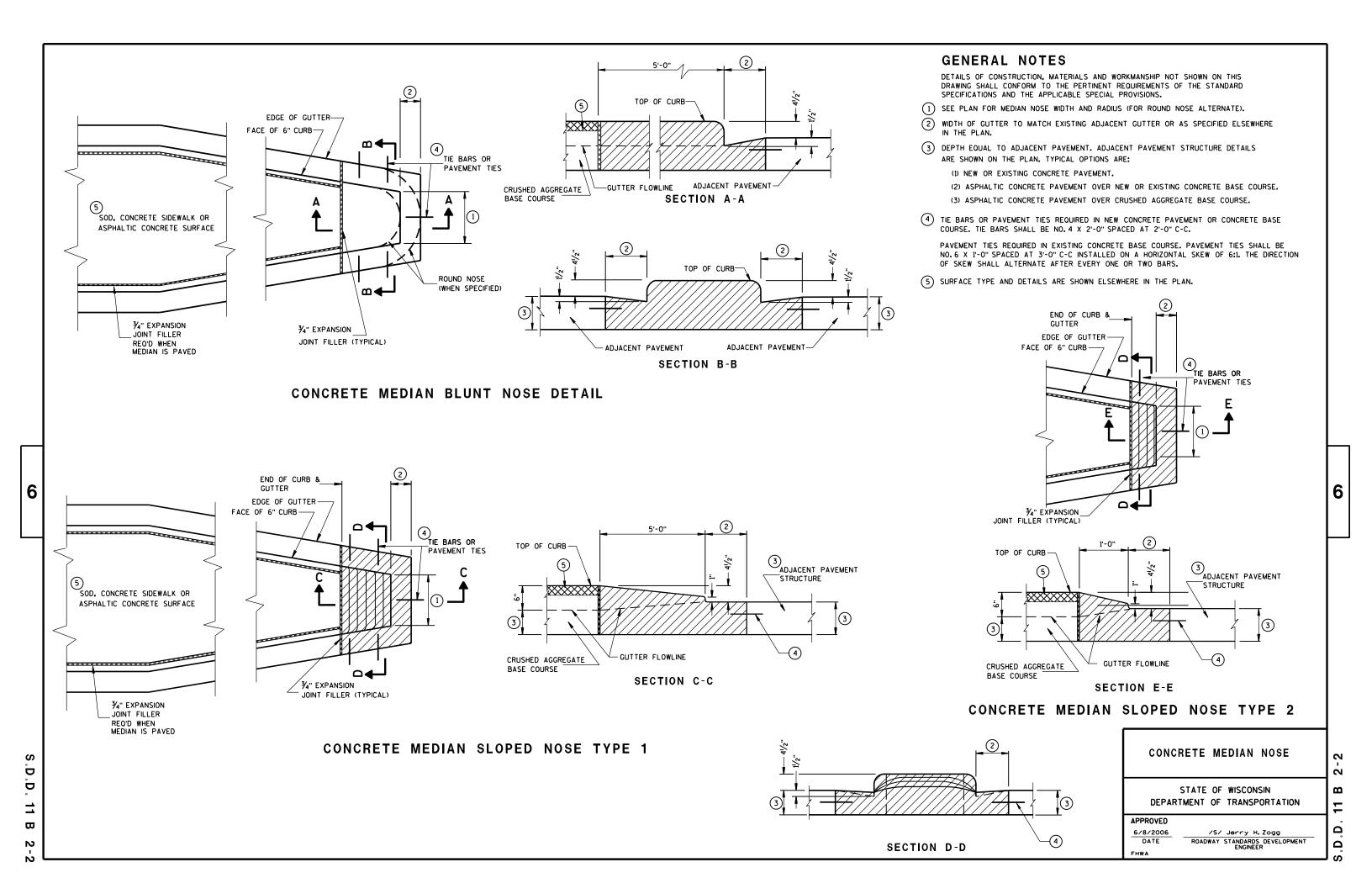
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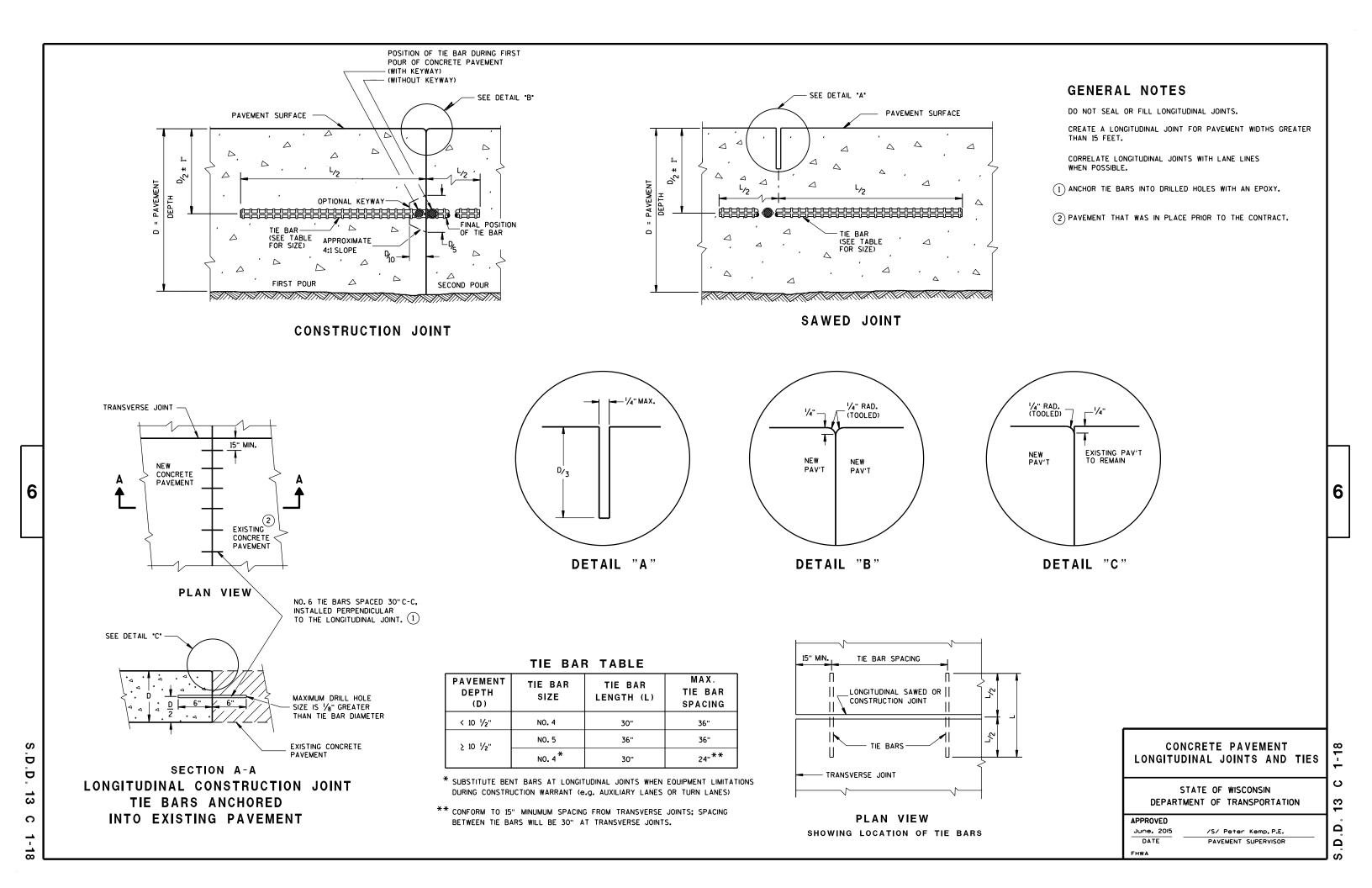
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CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT SEAL OR FILL CONTRACTION JOINTS.

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE FREE EDGE OF PAVEMENT.

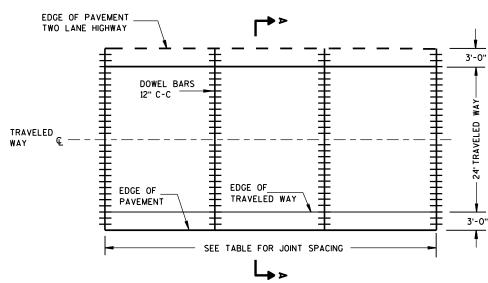
CONSTRUCTION JOINTS

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.

- 1 REFER TO TYPICAL CROSS SECTIONS FOR ADDITIONAL DETAILS.
- 2 MEASURE THE ENTIRE PAVED WIDTH INCLUDING THE PORTION(S) LABELED PAVED SHOULDER AS CONCRETE PAVEMENT.

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 ½", 6",6 ½"	NONE	12'
7",7 1/2"	1"	14'
8" , 8 ¹ / ₂ "	1 1/4"	15'
9",9 1/2"	1 1/4"	15'
10" & ABOVE	1 1/2"	15'



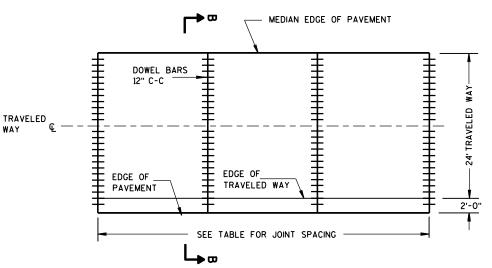
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CONTRACTION JOINT LAYOUT FOR TWO-LANE TWO-WAY HIGHWAY



PAVED

- 2'-0" PAVED

SHOULDER

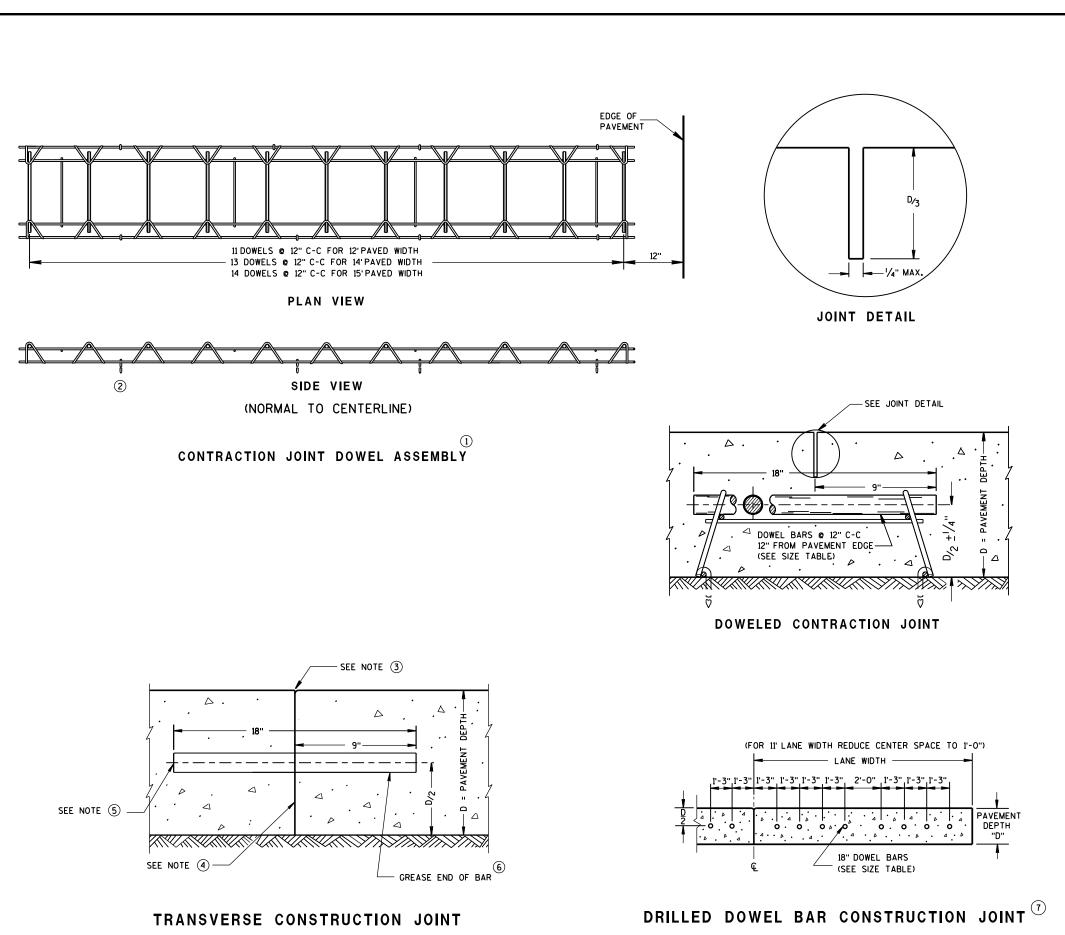
SHOULDER

CONTRACTION JOINT LAYOUT FOR DIVIDED HIGHWAY

RURAL DOWELED **CONCRETE PAVEMENT**

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION 6

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GENERAL NOTES

- (1) OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.
- ② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.
- 3 FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A 1/4-INCH RADIUS AT FORMED JOINTS.
- 4 PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.
- (5) INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO DRILLED DOWEL BAR CONSTRUCTION JOINT DETAIL.
- (6) APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.
- 7 ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS 1/8-INCH GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.

RURAL DOWELED CONCRETE PAVEMENT

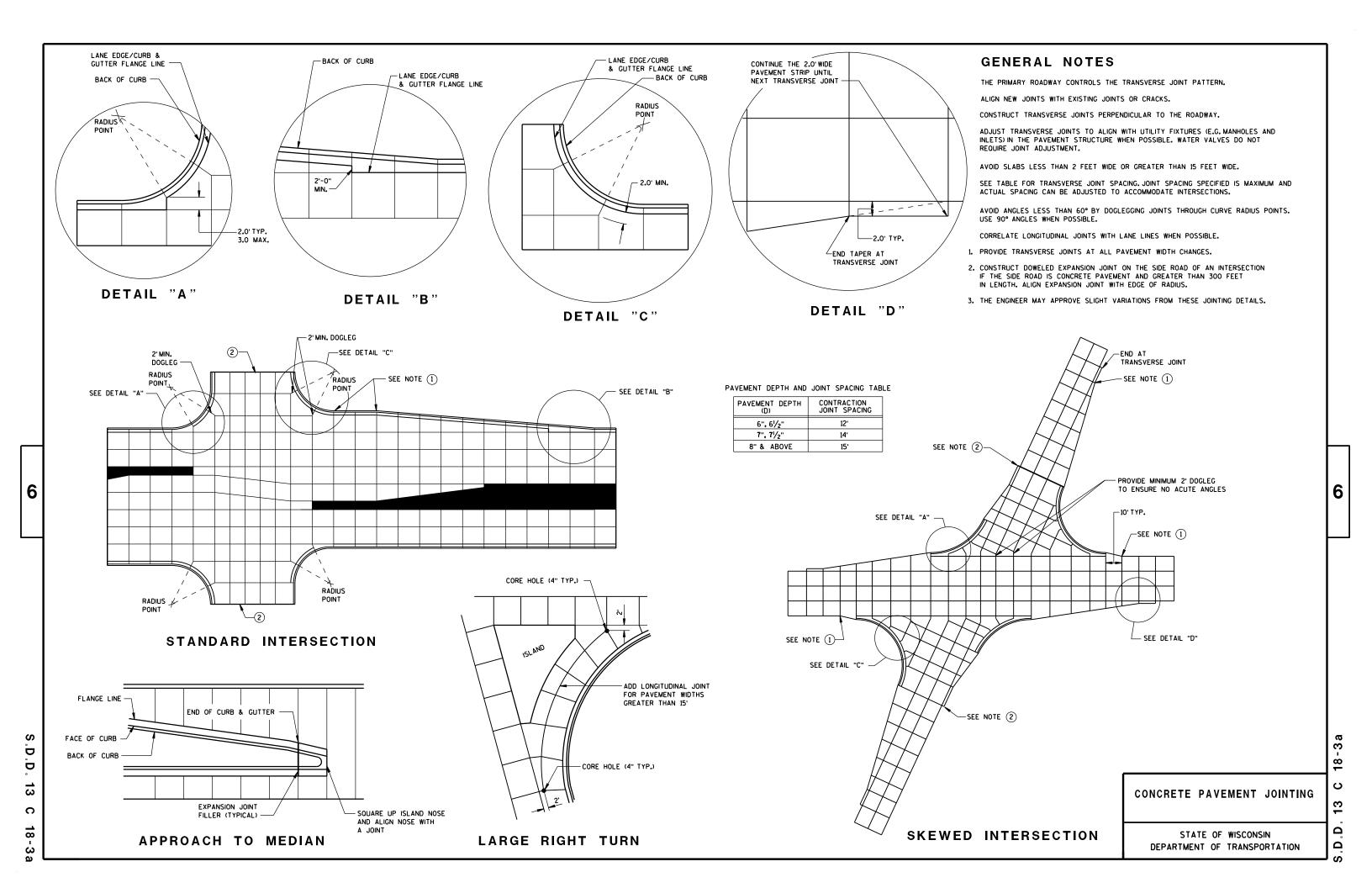
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

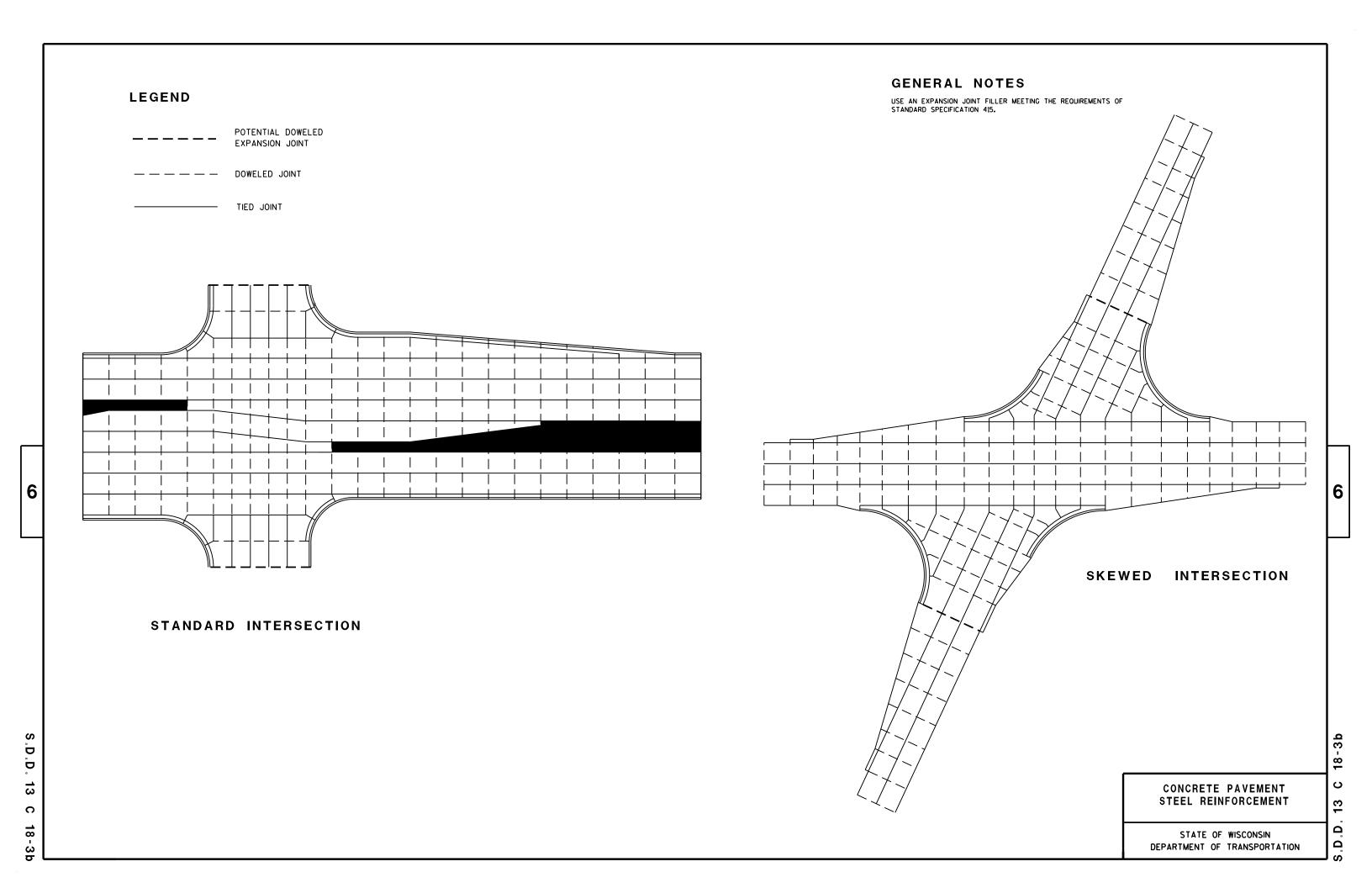
APPROVED

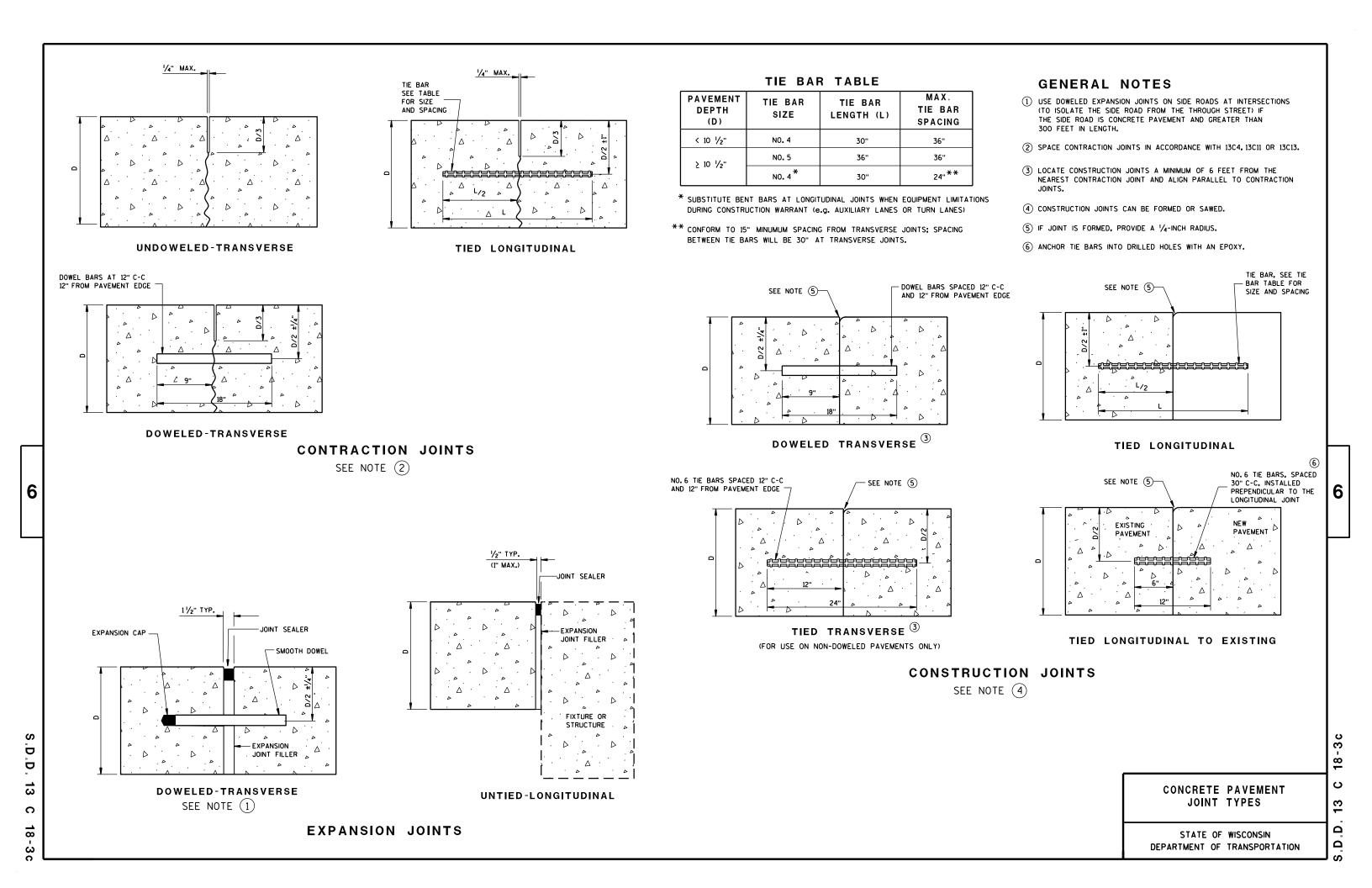
DATE PAVEMENT POLICY & DESIGN ENGINEER

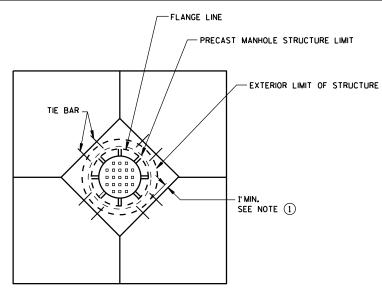
FHWA

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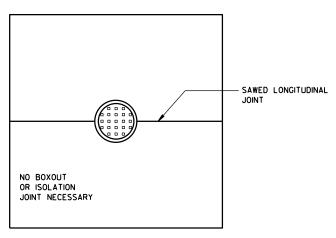




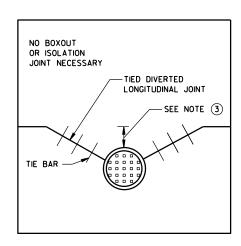




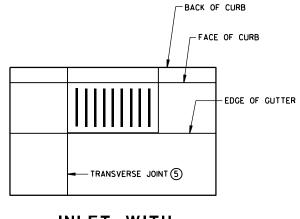
DIAGONAL MANHOLE BOXOUT FOR CONSTRUCTION JOINTS



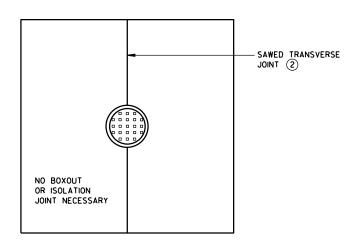
MANHOLE WITH LONGITUDINAL JOINT



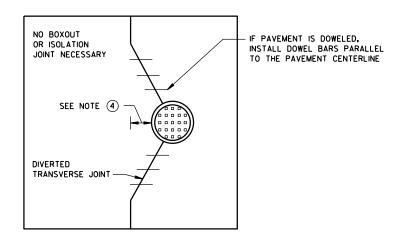
MANHOLE WITH DIVERTED LONGITUDINAL CONTRACTION JOINT



INLET WITH TRANSVERSE JOINT



MANHOLE WITH TRANSVERSE JOINT



MANHOLE WITH DIVERTED TRANSVERSE CONTRACTION JOINT

- 1 USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1-FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.
- 2 ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.
- (3) IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS. DIVERT THE LONGITUDIAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
- (4) IF DISTANCE FROM THE EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS 4 FEET OR LESS. REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.
- (5) ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.

CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED DATE

/S/ Peter Kemp, P.E. PAVEMENT SUPERVISOR FHWA

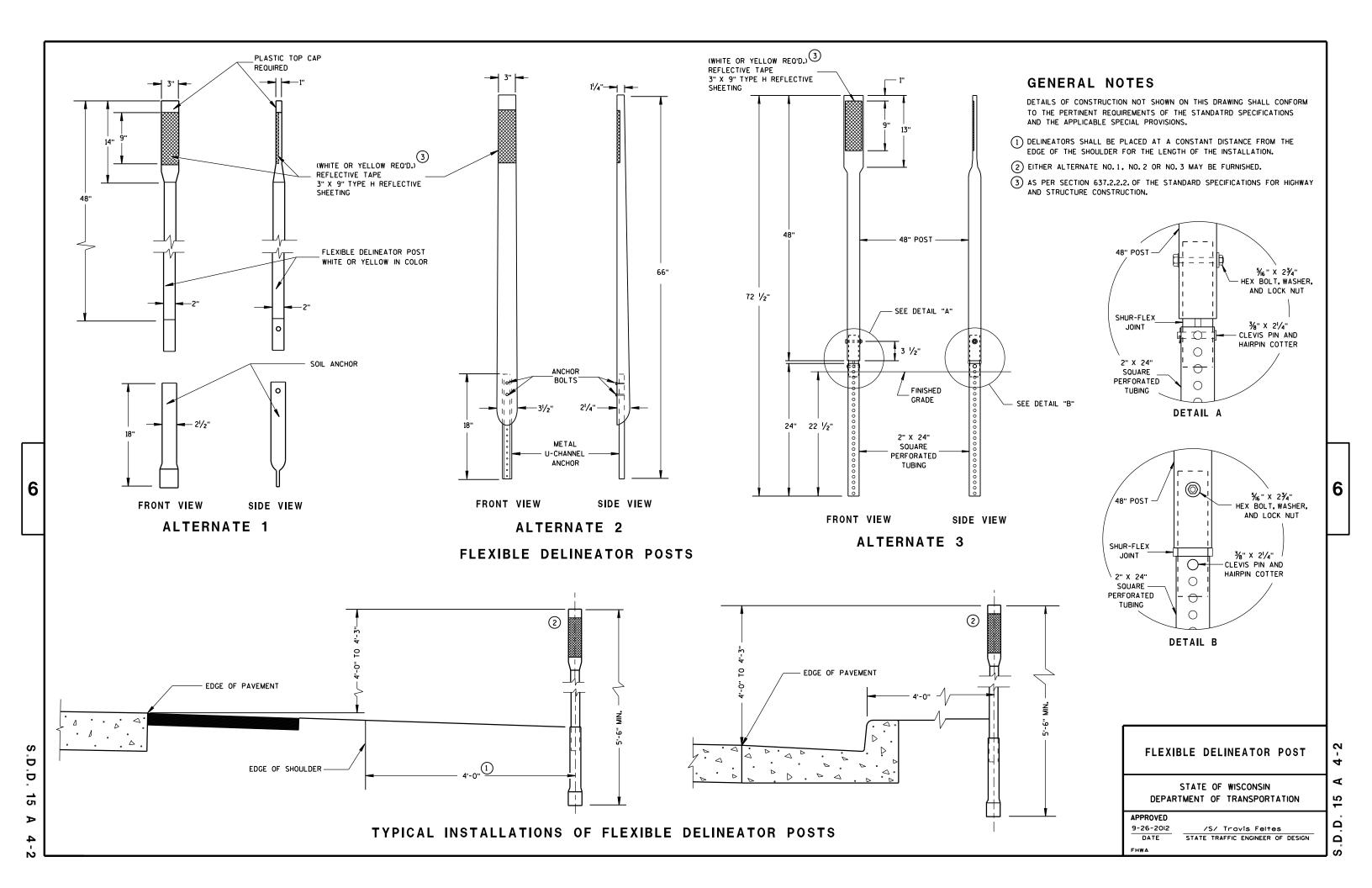
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THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

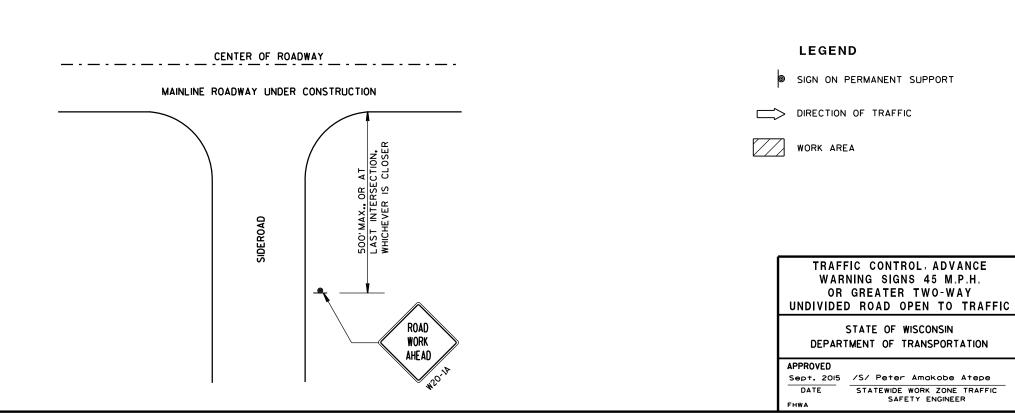
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- * PLACE ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



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SAFETY ENGINEER

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THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

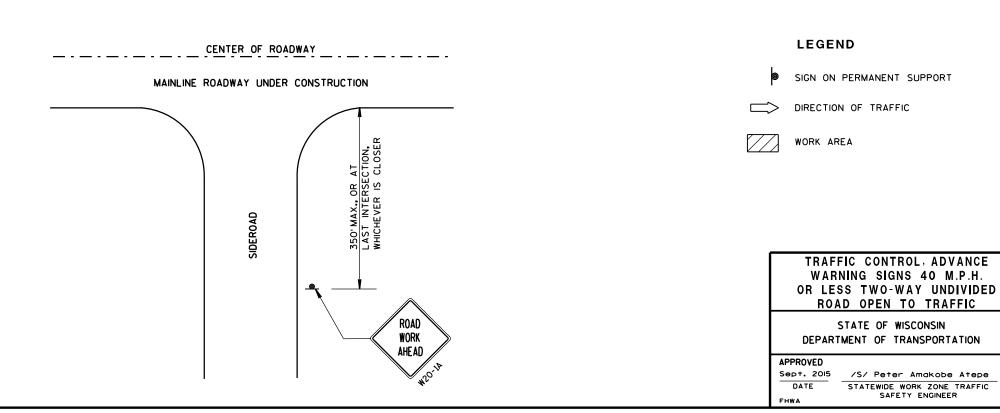
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"×36" SIGNS MAY BE USED INSTEAD OF 48"×48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

★ THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FT" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

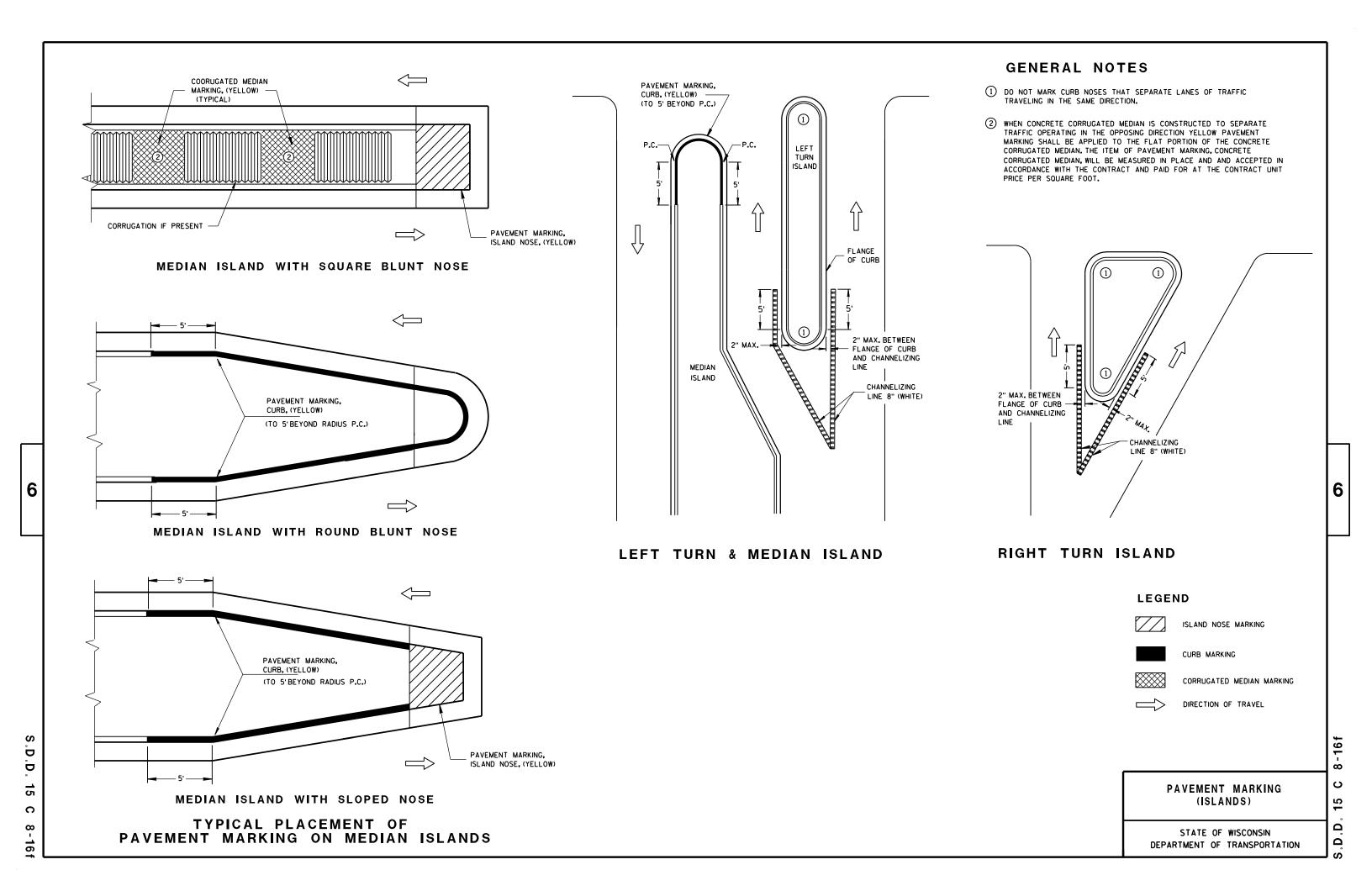


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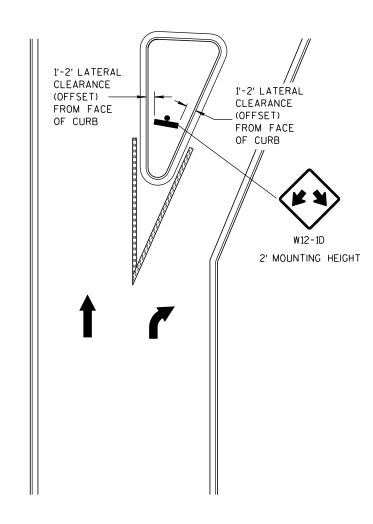
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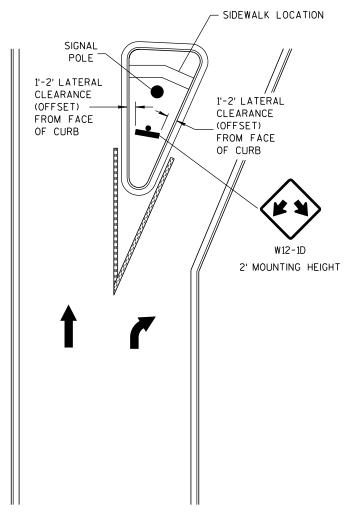








LARGE RIGHT TURN ISLAND



LARGE RIGHT TURN ISLAND
WITH SIGNAL POLE

SIGNAL POLE 2' MOUNTING HEIGHT OPTION #2 1'-2' LATERAL IF LATERAL CLEARANCE CLEARANCE 1'-2' LATERAL (OFFSET) NOT POSSIBLE CLEARANCE FROM FACE WITH OPTION #1 (OFFSET) OF CURB FROM FACE OF CURB 2' MOUNTING HEIGHT OPTION #1

SMALL RIGHT TURN ISLAND

DOUBLE ARROW WARNING SIGN PLACEMENT

DOUBLE ARROW
WARNING SIGN PLACEMENT

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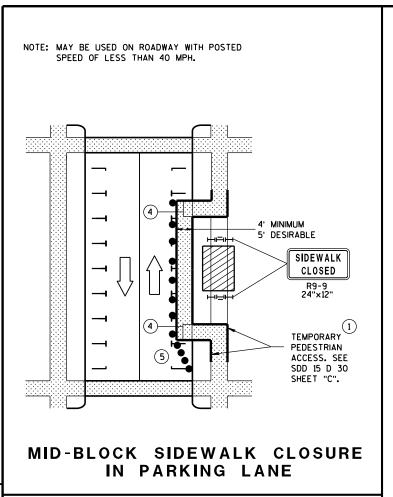
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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

DATE STATE TRAFFIC ENGINEER OF DESIGN





NOTE: LAYOUT SAME AS ABOVE. 4' MINIMUM 5' DESIRABLE SIDEWALK CLOSED RQ-Q TEMPORARY PEDESTRIAN ACCESS. SEE SDD 15 D 30 SHEET "C". SIDEWALK DIVERSION

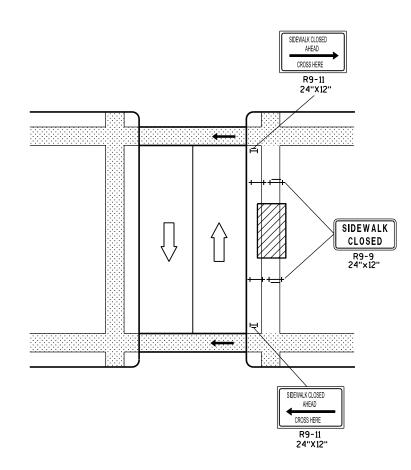
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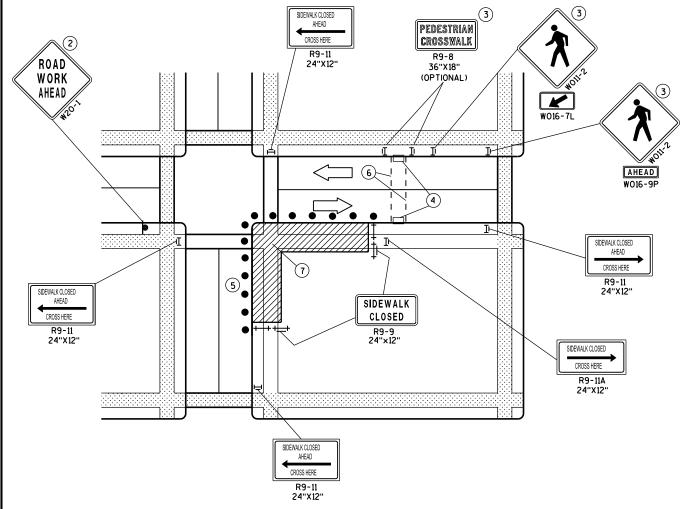
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MID-BLOCK SIDEWALK CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

- 1) IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.
- 2) "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.
- (3) IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND WO11-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.
- (4) TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".
- (5) DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
- (6) TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
- (7) LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN

LEGEND

SIGN ON PERMANENT SUPPORT

UNDER PEDESTRIAN TRAFFIC

TRAFFIC TRAFFIC CONTOL DRUM

DIRECTION OF

WORK AREA

PEDESTRIAN CHANNELIZATION DEVICE

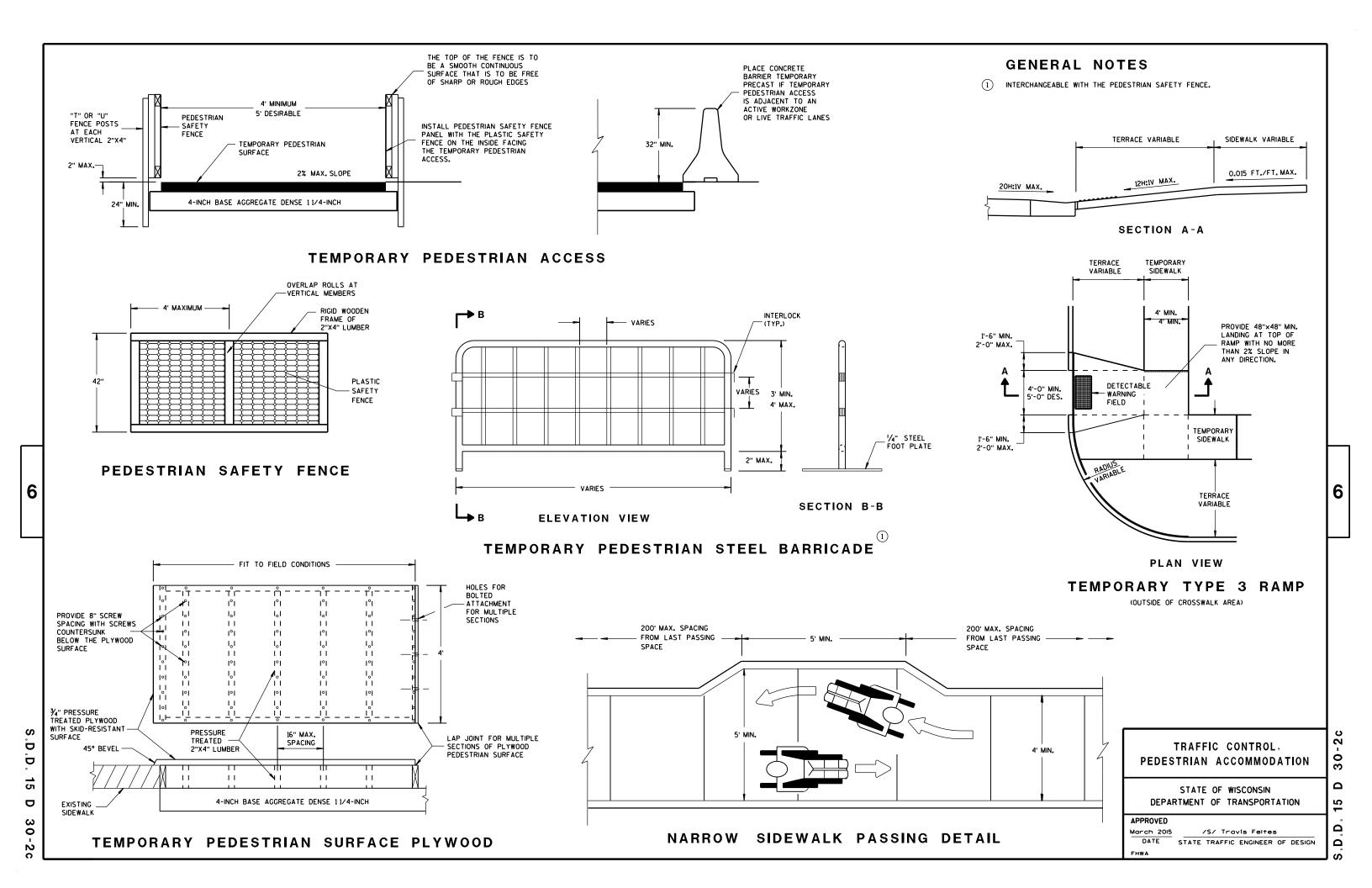
TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A. LOW-INTENSITY FLASHING)

TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION S 0 က Ω Ω

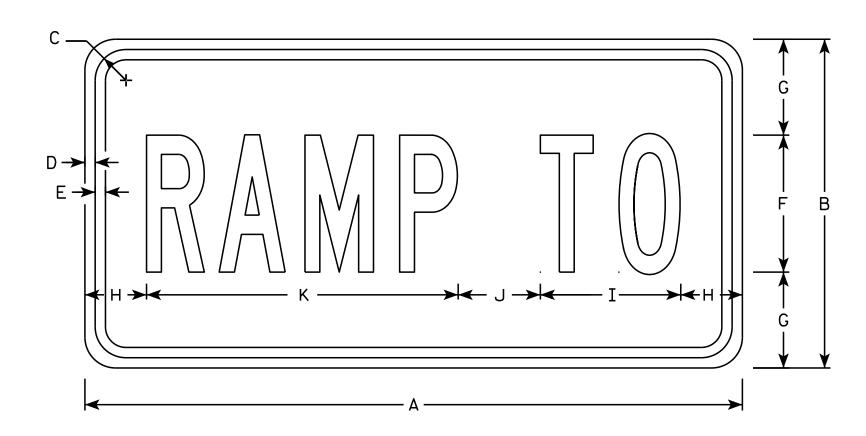
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- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	₩	X	Y	Z	Areo sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	5	3 ½	2 1/4	5 1/8	3	11 3/8																2.0
3	36	18	1 1/8	3/8	1/2	8	5	2 1/4	8 1/4	5	18 1/4																4.5
4																											1
5		·																									

PROJECT NO: 1009-44-66

HWY: VARIOUS

COUNTY: VARIOUS

SHEET NO:

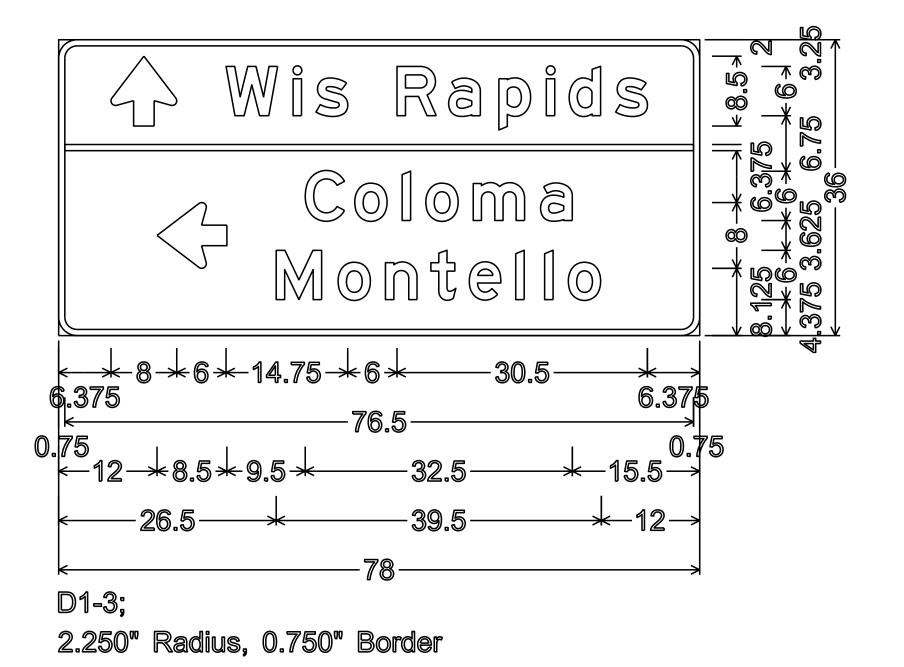
FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M410.DGN

PLOT DATE : 07-0CT-2015 14:51

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

PLOT SCALE : 5.582139:1.000000

WISDOT/CADDS SHEET 42



1. Sign is Type II - Type H Reflective

2. Color:

Background - Green Message - White

3. Message Series - E

PROJECT NO: 1009-44-66

HWY: VARIOUS

COUNTY: VARIOUS

PERMANENT SIGNING: STH 21, STH 22, STH 73 - INTERSECTION 3

SHEET NO:

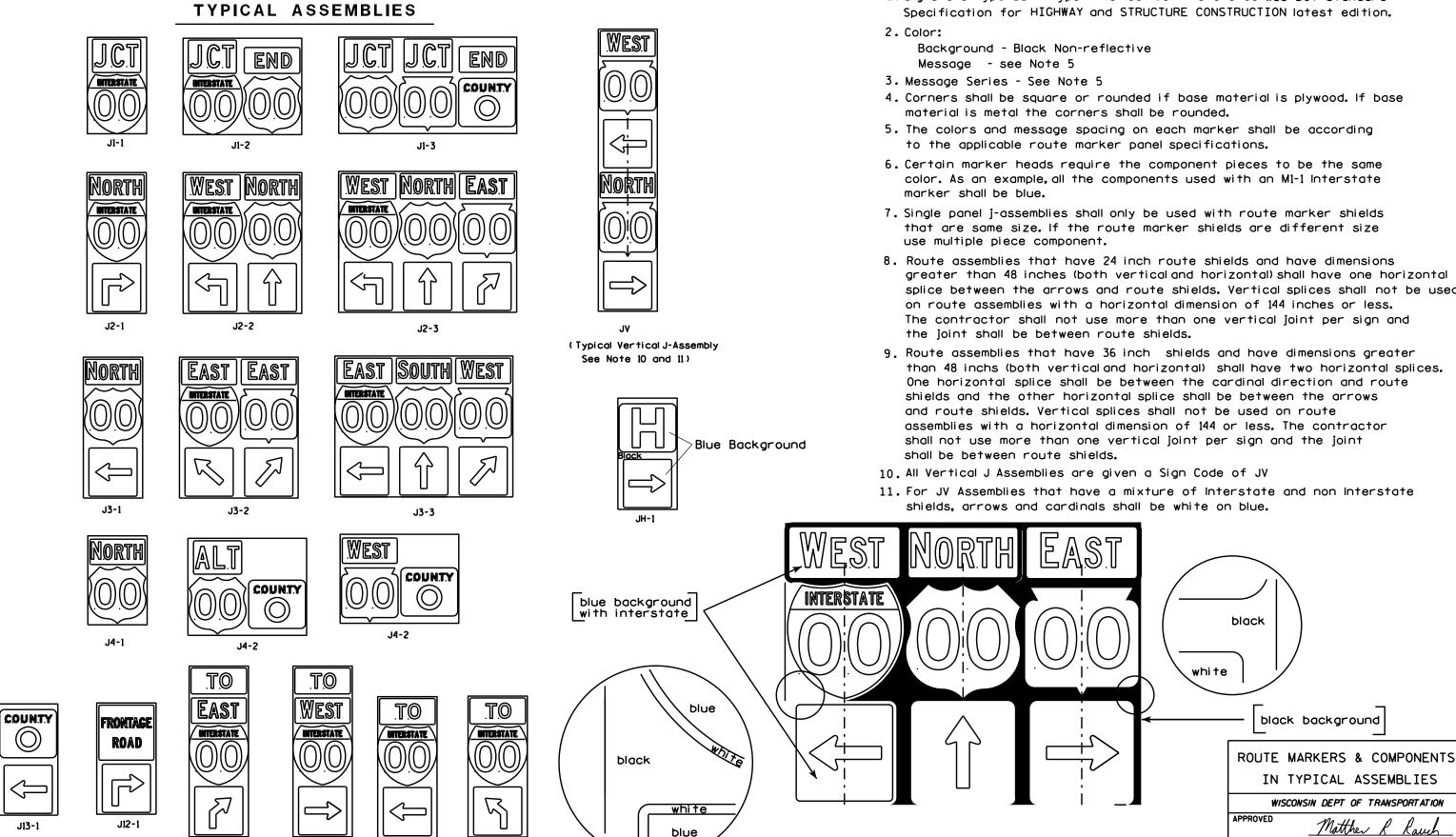
FILE NAME : C:\CAEfiles\Projects\tr_d4\4692co15.DGN

PLOT DATE : 07-0CT-2015 14:44

PLOT BY : \$8...plotuser...\$8 PLOT NAME :

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard

areater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.



PROJECT NO:

J32-1

J22-1

J23-1

J33-1

PLOT BY: mscsja

PLATE NO. __A2-15.8

DATE 2/06/14

SHEET NO:

URBAN ARFA



RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



5'-3"(生) D^{-1} Outside Edae of Gravel

White Edgeline Location

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where

there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

PLOT BY : mscj9h

GENERAL NOTES

- 1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is $7'-3''(\pm)$ or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A2-1S) is $7'-3''(\pm)$ or $6'-3''(\pm)$ per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is 5' - 3'' (\pm).
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (+) tolerance for mounting height is 3 inches.
- 8. Folding signs shall be mounted at a height of 5'-3'' (\pm) or as directd by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm) . The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3'' (\pm).

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

APPROVED

for State Traffic Engineer

DATE 7/23/15

PLATE NO. <u>A4-3.20</u>

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A43.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:21

COUNTY:

PLOT NAME :

PLOT SCALE: 99.237937:1.000000

WISDOT/CADDS SHEET 42



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

WISDOT/CADDS SHEET 42

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3'' (±) or 6'-3'' (±) per urban or rural detail respectively.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).
- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- *** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

URBAN AREA RURAL AREA (See Note 3) 2'Min - 4'Max (See Note 6) ₩E# FF# 6'-3"(±) 6'-3"(±) 7'-3"(±) ** Curb ********\ Flowline D **7000** White Edgeline D 11 White Edgeline, Location Outside Edae Location

2' Min - 4' Max (See Note 6) 6'-3"(±) Curb Flowline. -11

48" DIAMOND WARNING SIGN

HWY:

_ 26" 5 ' - 3 "(±) White Edgeline Location Outside Edge of Gravel 48" DIAMOND WARNING SIGN

COUNTY:

Outside Edge

of Gravel

	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRED		
	L	E	
* * *	Greater than 48" Less than 60"	12"	
	60" to 120"	L/5	l

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN (FOUR POSTS REQUIRE	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

of Gravel

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

Matther

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:23

PLOT SCALE: 107.021305:1.000000

WISDOT/CADDS SHEET 42

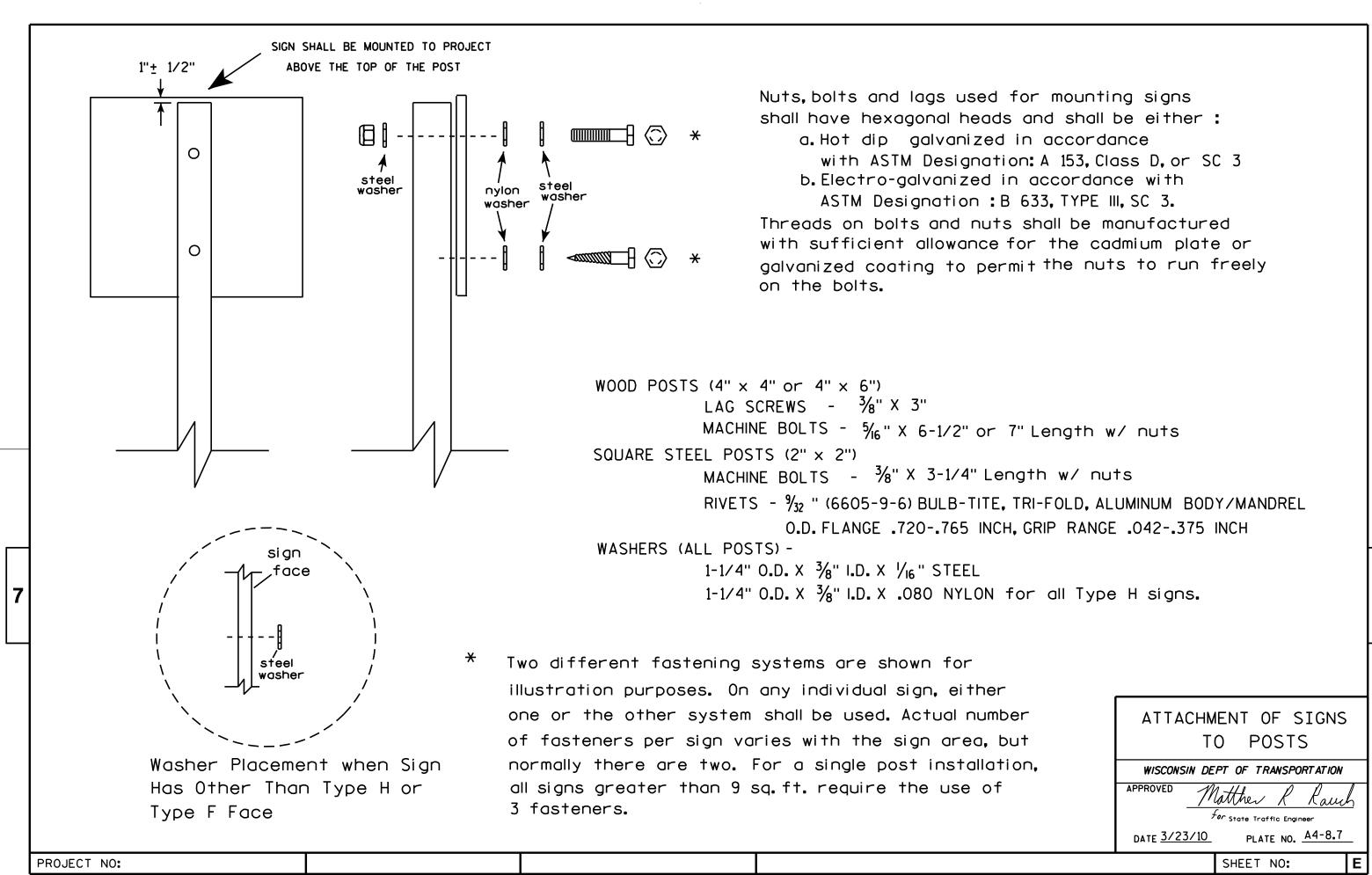
PLOT NAME :

PLOT BY: mscj9h

WISCONSIN DEPT OF TRANSPORTATION APPROVED

For State Traffic Engineer

PLATE NO. 44-4.14 DATE 7/23/15





PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

For State Traffic Engineer



- 1. Sign is Type II See Note 6 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White & Black - See Note 6 Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
- 6. Permanent Signs Background - Type H Reflective Detour or temporary Signs Background - Reflective

	BLACK BLACK
Metric equivalent for this sign is:	>

HWY:

SIZE 600 mm X 600 mm 900 mm X 900 mm 900 mm X 900 mm 900 mm X 900 mm

PROJECT NO:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1																												ļ
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 %	11 ½	1	1 1/8	11 1/4	21 1/8											4.0	. 36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 %	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5	12 5/8	17 1/8	1 1/2	2 1/8	16 1/8	33											9.0	. 81

COUNTY:

STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 3/20/02 PLATE NO. M1-6.9

SHEET NO:

FILE NAME : C:\Users\Projects\tr_stdplate\M16.DGN

PLOT DATE: 13-OCT-2005 14:55

PLOT BY : DITJPH

PLOT NAME :

PLOT SCALE : 6.715871:1.000000

WISDOT/CADDS SHEET 42







MP3-1









HWY:



NOTES

- 1. All Signs Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

5. M3-1 thru M3-4 Background - White Message - Black

MB3-1 thru MB3-4 Background - Blue

Message - White

MK3-1 thru MK3-4 Background - Green

Message - White

MM3-1 thru MM3-4 Background - White

Message - Green

MN3-1 thru MN3-4 Background - Brown

Message - White

MP3-1 thru MP3-4 Background - White

Message - Blue

6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1 1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 1/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

COUNTY:

STANDARD SIGNS M3-1 thur M3-4 **SERIES**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 10/15/15 PLATE NO. M3-1.14

Ε

SHEET NO:

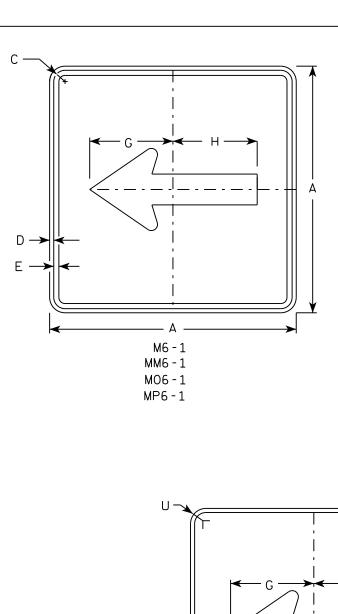
FILE NAME · C·\CAFfiles\Projects\tr stdolote\M31 DCN

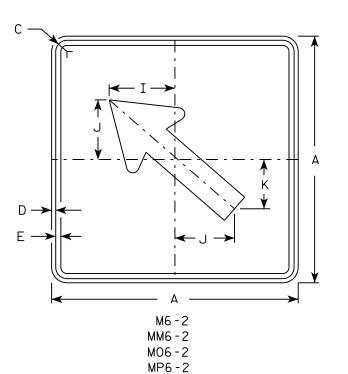
PROJECT NO:

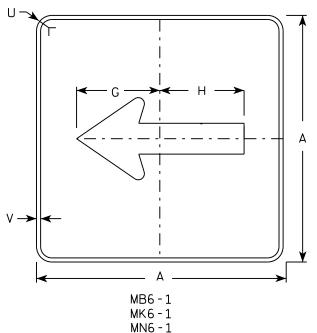
PLOT DATE . 01-DEC-2015 17:54

PLOT RY . \$\$ plotuser \$\$ PLOT NAME :

PLOT SCALE . 11 675051.1 000000

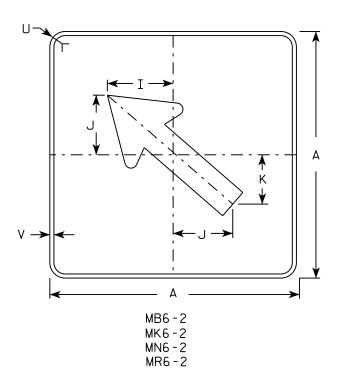






MR6-1

HWY:



NOTES

- 1. Signs are Type II Type H except as Shown
- 2. Color:

Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background White

Message - Black

MB6-1 and MB6-2 Background - Blue

Message - White

MK6-1 and MK6-2 Background - Green

Message - White

MM6-1 and MM6-2 Background - White

Message - Green

MN6-1 and MN6-2 Background - Brown

Message - White

M06-1 and M06-2 Background - Orange - Type F Reflective

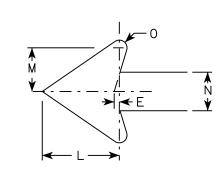
Message - Black

MP6-1 and MP6-2 Background - White

Message - Blue

MR6-1 and MR6-2 Background - Brown

Message - Yellow



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	٥	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1 1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 %	5	4 1/4	5 1/4	3	2 %	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew & Rawl For State Traffic Engineer

Ε

DATE 10/15/15 PLATE NO. M6-1.15

SHEET NO:

FILE NAME · C·\CAFfiles\Projects\tr stdplote\M61 DCN

PROJECT NO:

PLOT DATE . 01-DEC-2015 17:57

PIOT RY . \$\$ plotuser \$\$ PIOT NAMF :

PLOT SCALE . 11 675051.1 000000

NOTES 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. 2. Color: Background - Red Message - White 3. Message Series - C R1-1 SIZE A STANDARD SIGN 30 5/8 10 12 1/2 45° 12 3/4 5.18 2S 30 5/8 12 1/2 45° 12 3/4 10 5.18 R1-1 2M 36 3/4 12 15 45° 15 % 7.46 3/4 15 3/8 12 45° 36 15 7.46 WISCONSIN DEPT OF TRANSPORTATION 45° 20 1/2 48 16 20 13.25 APPROVED Matthew & Kauch 5 48 16 20 45° 20 1/2 13.25 3/8 7 3/4 45° 7 3/4 1.86 18 6 For State Traffic Engineer 12 1/4 4 45° 5 1/8 0.78 DATE <u>11/12/15</u> PLATE NO. _____R1-1.13 COUNTY: SHEET NO: PROJECT NO: HWY: PLOT SCALE • 4 378143•1 000000

FILE NAME · C·\CAFfiles\Projects\tr stdplote\R11 DGN

PLOT DATE . 01-DEC-2015 18:07

PINT RY . \$\$ plotuser \$\$ PINT NAMF :

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - See note 5

3. Message Series - C

PLOT NAME :

- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. The border strip and word message are reflectorized red.

A	
c —	G
	¥ F ¥
E	
D.	
\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\\	
R1-2	

SIZE	Α	В	С	D	E	F	G	н	I	J	K	L	М	N	0	Р	0	R	S	T	U	V	W	Х	Y	Z	Area sq. ft.
1	30	26	1 1/2	5/8	4	2 1/2	6 3/8	7 ⁄8	4	3 %																	2.71
25	36	31	2	3/4	5	3	7 3/4	1 1/4	4 3/4	4 3/8																	3.88
2M	48	42	3	1	6	4	9 3/4	2	6 1/4	5 %																	7.00
3	48	42	3	1	6	4	9 3/4	2	6 1/4	5 %																	7.00
4	48	42	3	1	6	4	9 3/4	2	6 1/4	5 %																	7.00
5	60	52	3	1 1/2	8	5	13	2 1/2	7 1/8	7 1/4																	10.83
6																											
7	18	15 1/2	1	3/8	2 1/2	1 1/2	3 1/8	5/8	2 3/8	2 1/4																	0.97

COUNTY:

STANDARD SIGN R1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew & Rauch

 f_{or} State Traffic Engineer

3/14 PLATE NO. R1-2.12

DATE 10/13/14 PLA

SHEET NO:

311221

PROJECT NO:

HWY:

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Red Message - White

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

	C C C C C
R1-3P	

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6																0.75
25	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6																1.5
2M	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 ¾																1 . 5
3	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
4	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5
5	30	12	2 1/4		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5

COUNTY:

STANDARD SIGN R1-3P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch

DATE 9/24/15

PLATE NO.R1-3P.2

Ε

FILE NAME . C.\CAFfiles\Projects\tr stdolote\R13P DGN

HWY:

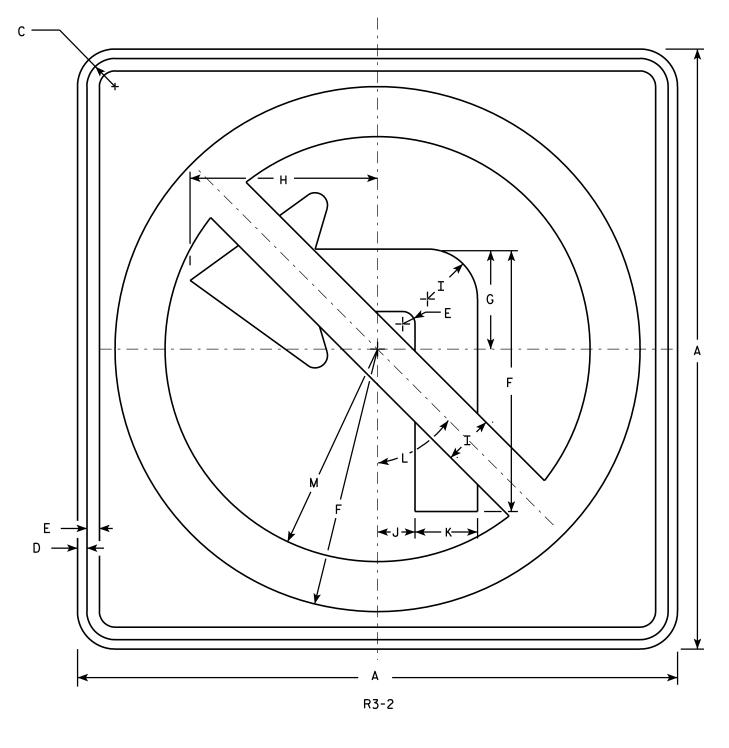
PROJECT NO:

PLOT DATE . 01-DEC-2015 18:08

PLOT RY . \$\$ plotuser \$\$ PLOT NAMF :

SHEET NO:

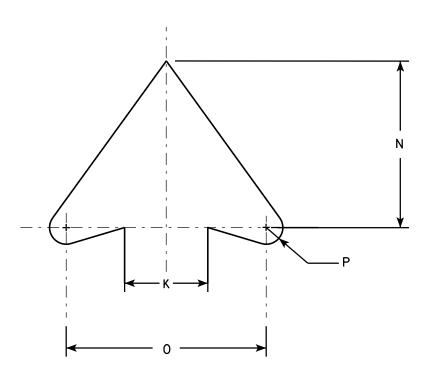
PLOT SCALE . 1 945842.1 000000



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	₩	X	Y	Z	Area sq. ft.
1	24		1 1/8	3⁄8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
25	24		1 1/8	3∕8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2M	36		1 1/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 ½	9	3/4											9.0
3	36		1 1/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 ½	9	3/4											9.0
4	36		1 1/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 ½	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1										_	16.0

COUNTY:

STANDARD SIGN R3-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

 $f_{\it or}$ State Traffic Engineer

DATE 12/08/10

PLATE NO. R3-2.10

SHEET NO:

HWY:

PROJECT NO:

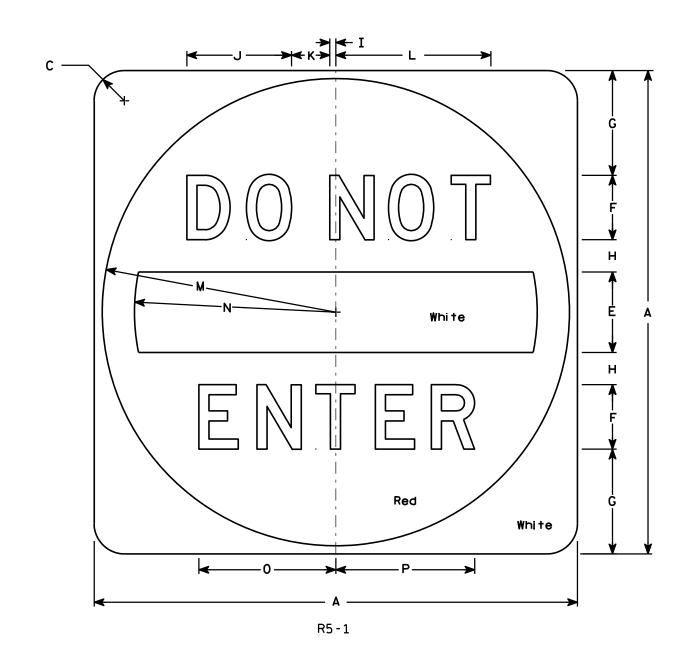
PLOT NAME :

<u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See detail Message - White - Type H Reflective

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but when base material is metal, the cornors shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1																											
25	30		1 1/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 %	14 1/2	12 1/2	8 1/2	8 %											6.26
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 ¾											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 %	14 1/2	23 ½	20	12 3/4	12 1/8											16.0

COUNTY:

STANDARD SIGN R5-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 12/17/10

10 PLATE NO. R5-1.15

Р

PLOT NAME :

HWY:

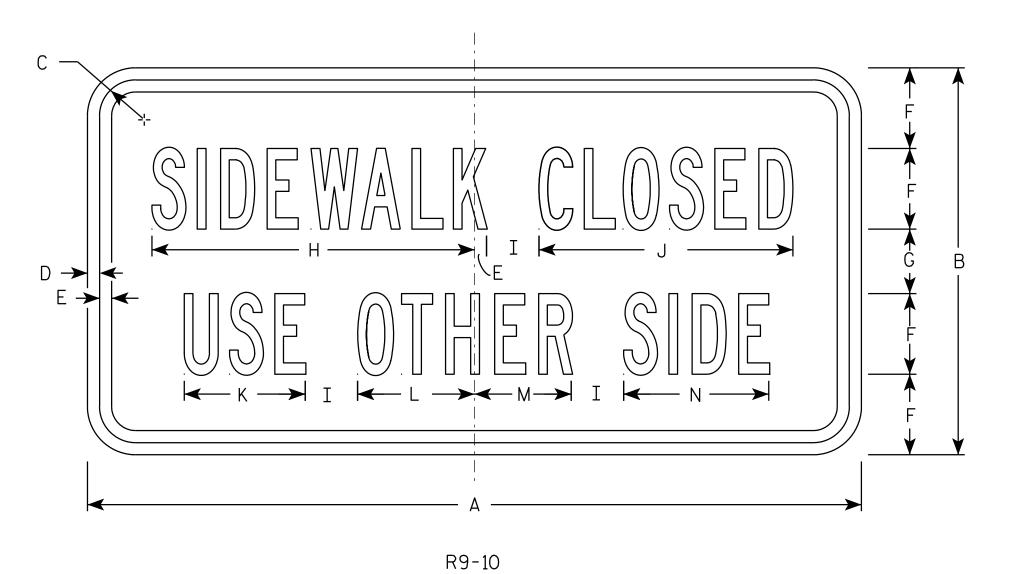
PROJECT NO:



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE N 0 Α 0 3/8 3/8 1 1/8 2 1/2 1 % 7 1/8 3 3/4 3 5/8 2 24 12 4 1/2 2.0 2S 2 3/4 3/4 3/4 3 1/4 15 3/4 7 1/2 7 1/4 48 24 5 9 8.0 2M 3 1/4 15 3/4 7 1/2 48 24 2 3/4 8.0 3 4 5

COUNTY:

STANDARD SIGN R9-10

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 8/16/2012

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\R910.DGN

PROJECT NO:

HWY:

PLOT DATE: 16-AUG-2012 09:37

PLOT BY: mscsja

PLOT NAME :

PLOT SCALE: 2.977140:1.000000

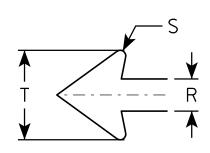
WISDOT/CADDS SHEET 42

PLATE NO. R9-10.5

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C except Size 1 is Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



C		F Y
		G F Y B
D ->		H
		F V

R9-11

HWY:

SIZE	Α	В	С	D	Ε	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	v	W	X	Y	Z	Area sq. ft.
1	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5%	1 1/2	7 %	3 1/2	9 1/4	6 %	5 1/8		1	1/8	2 3/4							2.0
2S	48	30	2 3/4	3/4	3/4	4	2 1/2	3 1/2	20 1/2	1 1/4	3	16 3/8	7 1/4	18 1/2	14	11 1/8		2	3/8	5 1/2							10.0
2S 2M	48	30	2 3/4	3/4	3/4	4	2 1/2	3 1/2	20 1/2	1 1/4	3	16 3/8	7 1/4	18 1/2	14	11 1/8		2	3/8	5 ½							10.0
3																											
4																											
5																											

COUNTY:

STANDARD SIGN R9-11

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 8/17/2012

PLATE NO. R9-11.2

SHEET NO:

PLOT DATE: 17-AUG-2012 10:32

PLOT BY: mscsja

PLOT NAME :

PLOT SCALE : 5.954280:1.000000

WISDOT/CADDS SHEET 42

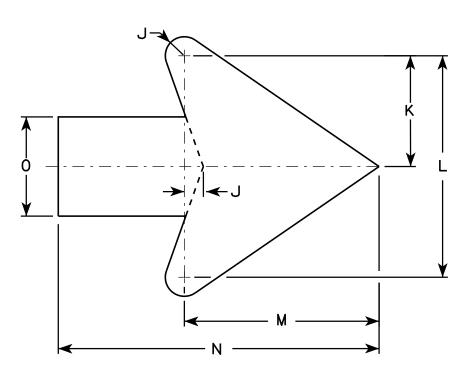
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\R911.DGN

PROJECT NO:

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

SIZE	Α	В	С	D	E	F	G	н	I	J	K	L	M	N	0	Ρ	0	R	S	T	U	V	W	X	Y	Z	Areo sq. ft.
1																											
25	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
2M	24		1 1/8	1/2	3/8		8	4	9 1/2	3/8	3 3/8	7 1/4	6 3/8	10 3/8	3 1/4												4.0
3	30		1 3/8	1/2	5/8		10	5	11 1/8	3/4	4 1/2	9	7 1/8	13	4												6.25
4	36		1 3/8	1/2	5/8		12	6	14 1/4	1	5 ½	10 1/8	9 %	15 ¾	4 3/4												9.0
5	48		2 1/4	₹4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0

COUNTY:

W12-1D

STANDARD SIGN W12-1D

WISCONSIN DEPT OF TRANSPORTATION

Fer State Traffic Engineer DATE 3/13/13 PLATE NO. W12-1D.15

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W121D.DGN

PROJECT NO:

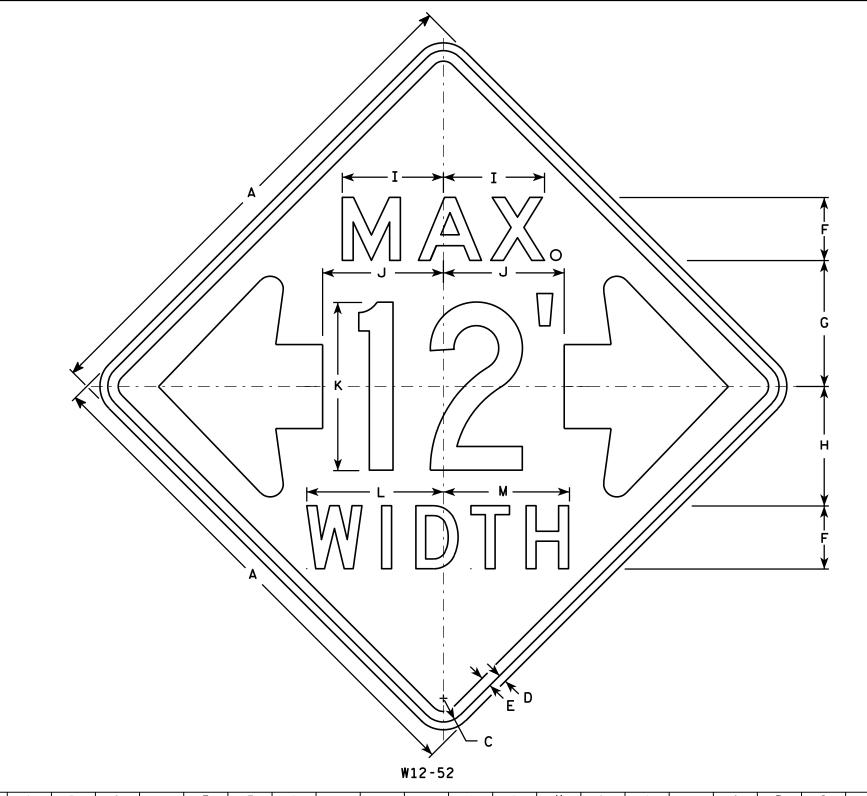
HWY:

PLOT DATE: 13-MAR-2013 13:26

PLOT BY: mscj9h

PLOT NAME :

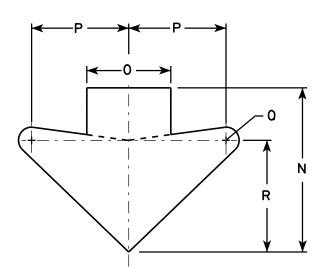
PLOT SCALE: 4.713802:1.000000



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. The top line is series E, the numerals are series C, and the bottom line is series D.
- 6. Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

CT TE			T T					ш			1/		1.4		_		_		_					_ v	·	7	Area
SIZE	Α	R	L	ַט	E	F	G	Н	l I	J	K	L	M	N	U	P	U	R	>	l	U	V	W	<u> </u>	T		Area sq. ft.
1																											
2S	48		2 1/4	3∕4	1	6	12	11 3/8	9 %	11 1/2	16	13	12	15 %	8	9 1/4	1 1/4	10 %									16.0
2M	48		2 1/4	3∕4	1	6	12	11 3/8	9 %	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 %									16.0
3																											
4																											
5																											

COUNTY:

STANDARD SIGN W12-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 3/16/11 PLATE NO. W12-52.7

SHEET NO:

HWY:

PROJECT NO:

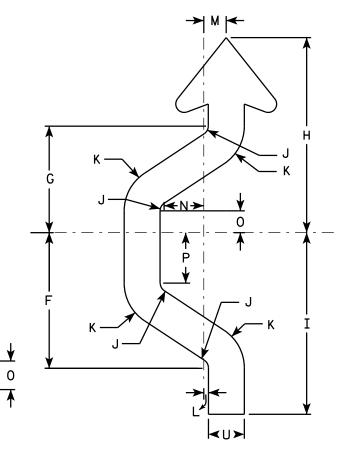
PLOT NAME :



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W24-1R is the same as W24-1L except reversed along the vertical centerline.



Arrow D	etail
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SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Ρ	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft
1																											
25	36		1 3/8	5/8	1/2	11 3/8	8 1/8	16 3/8	15 1/4	3/4	3 3/4	3/8	2	3 %	1 1/8	4 1/4		5 1/2	3 1/2	5/8	3						9
2M	36		1 3/8	5/8	1/2	11 3/8	8 1/8	16 3/8	15 1/4	3/4	3 3/4	3/8	2	3 %	1 1/8	4 1/4		5 1/2	3 1/2	5/8	3						9
3																											
4																											
5																											

COUNTY:

W24-1L

STANDARD SIGN W24-1 L & R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 9/25/2013 PLATE NO. W24-1.3

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W241L.DGN

HWY:

PROJECT NO:

W24-1R

PLOT DATE: 25-SEP-2013 13:00

Arrowhead Detail

PLOT BY: mscsja

PLOT NAME :

PLOT SCALE: 10.702132:1.000000

WISDOT/CADDS SHEET 42

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

c —	
D -> E ->	
M —	→ I ← I
N	Н — Н
Α	· · · · · · · · · · · · · · · · · · ·
WO:	1-6

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Areg sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 ¾													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 ¾													12.5

COUNTY:

STANDARD SIGN WO1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch
For State Traffic Engineer

DATE <u>11/18/13</u>

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W016.DGN

HWY:

PROJECT NO:

PLOT DATE: 28-FEB-2014 11:37

PLOT NAME :

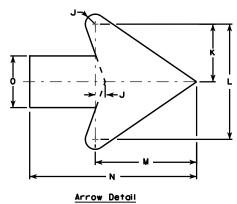
PLOT BY: mscj9h

PLOT SCALE: 5.837526:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



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<u> </u>
├ N ────
Arrow Datoil

SIZE	Α	В	С	D	Ε	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Areg sq. ft.
1	36		1 3/8	1/2	5/8		12	6	14 1/4	1	5 ½	10 1/8	9 %	15 ¾	4 3/4												9.0
2S	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0
2M	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0
3	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0
4	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4												16.0
5	48		2 1/4	3/4	1		16	8	19	1 1/4	7 1/4	14 1/2	12 3/4	21	6 1/4						·						16.0

COUNTY:

W012-1D

STANDARD SIGN WO12-1D

WISCONSIN DEPT OF TRANSPORTATION

DATE 11/20/13 PLATE NO. WO12-1D.1

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\W0121D.DGN

HWY:

PROJECT NO:

PLOT DATE: 28-FEB-2014 11:38

PLOT NAME :

PLOT BY: mscj9h

WISDOT/CADDS SHEET 42

PLOT SCALE: 9.057895:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

W057-52

* See note 5

SIZE	Α	В		С	D	E	F	G	Н	I	J	К	L	M	N	0	Ρ	0	R	S	T	U	v	W	Х	Y	Z	Area sq. ft.
1	36	24	4 1	1/8	3/8	1/2	6	4 1/2	3	4 3/4	14 %	10 %	11 3/8	2	13													6.0
2S	48	30	5 1	3/8	1/2	5/8	8	7	6	6 %	19 1/2	14	15	2 3/4	17 3/8													12.0
2M	48	30	5 1	3/8	1/2	5/8	8	7	6	6 %	19 1/2	14	15	2 3/4	17 3/8													12.0
3	48	30	5 1	3/8	1/2	5/8	8	7	6	6 %	19 1/2	14	15	2 3/4	17 3/8													12.0
4	48	30	5 1	3/8	1/2	5/8	8	7	6	6 %	19 1/2	14	15	2 3/4	17 3/8													12.0
5	48	30	5 1	3/8	1/2	5/8	8	7	6	6 3/8	19 1/2	14	15	2 3/4	17 3/8													12.0

COUNTY:

STANDARD SIGN W057-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 11/20/13

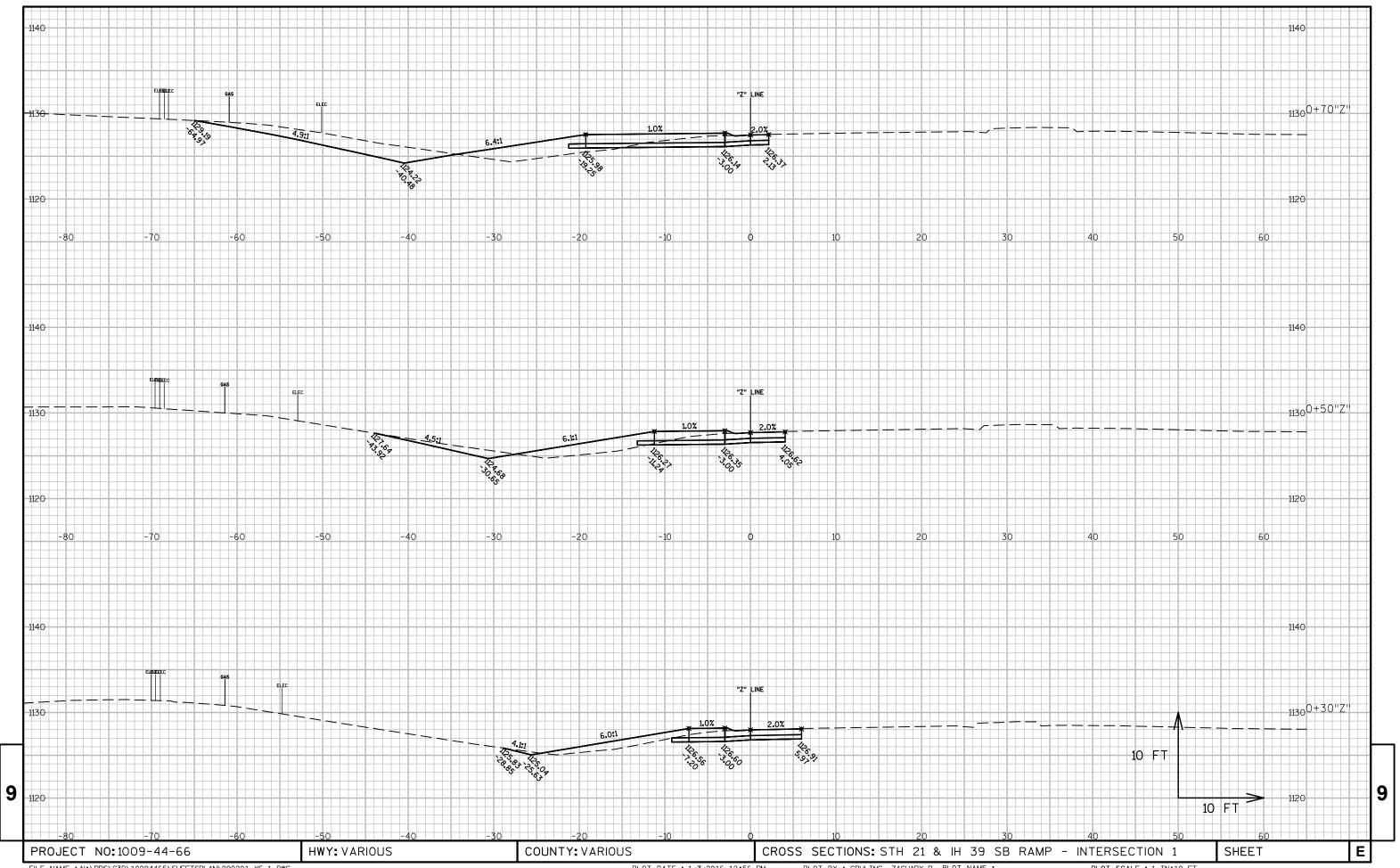
SHEET NO:

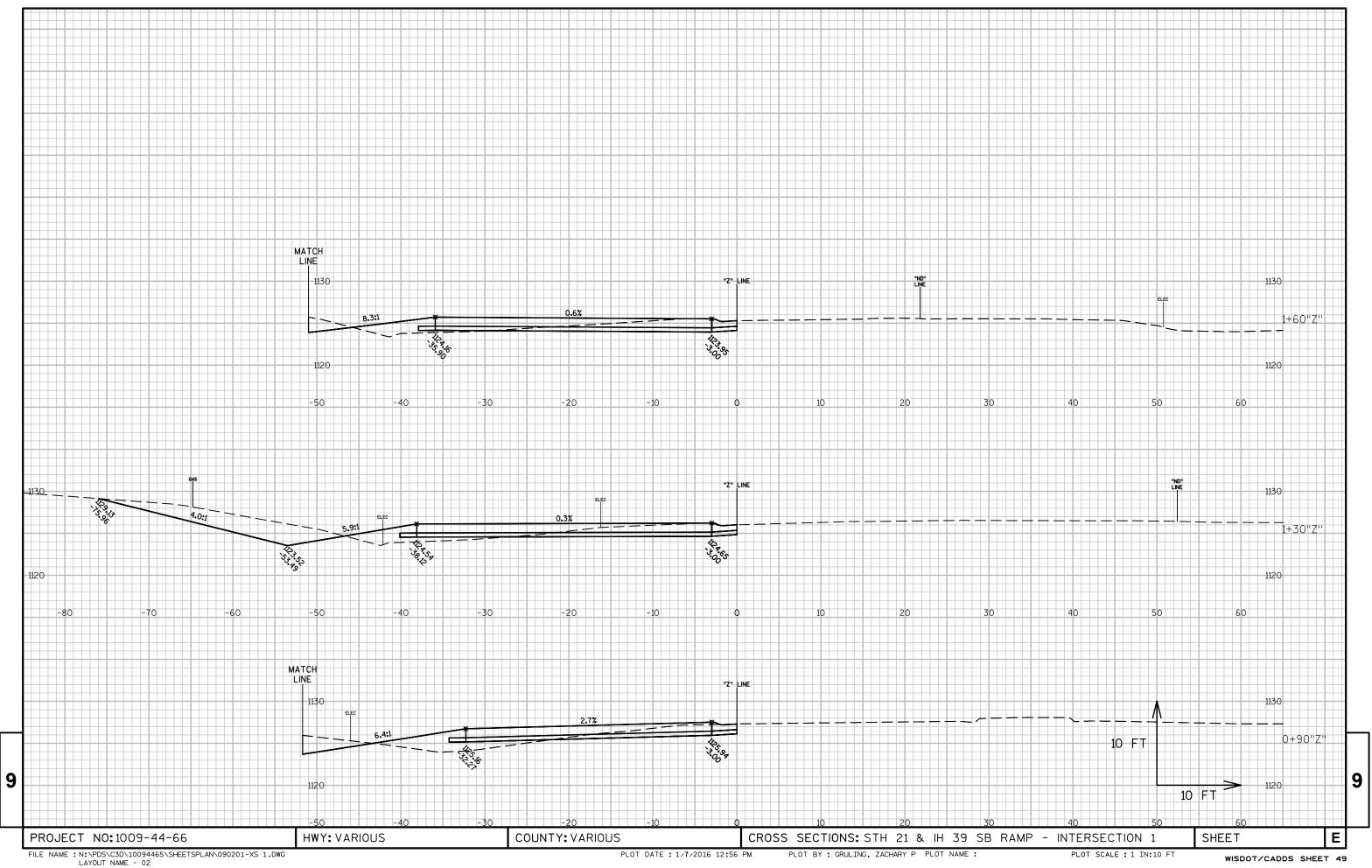
HWY:

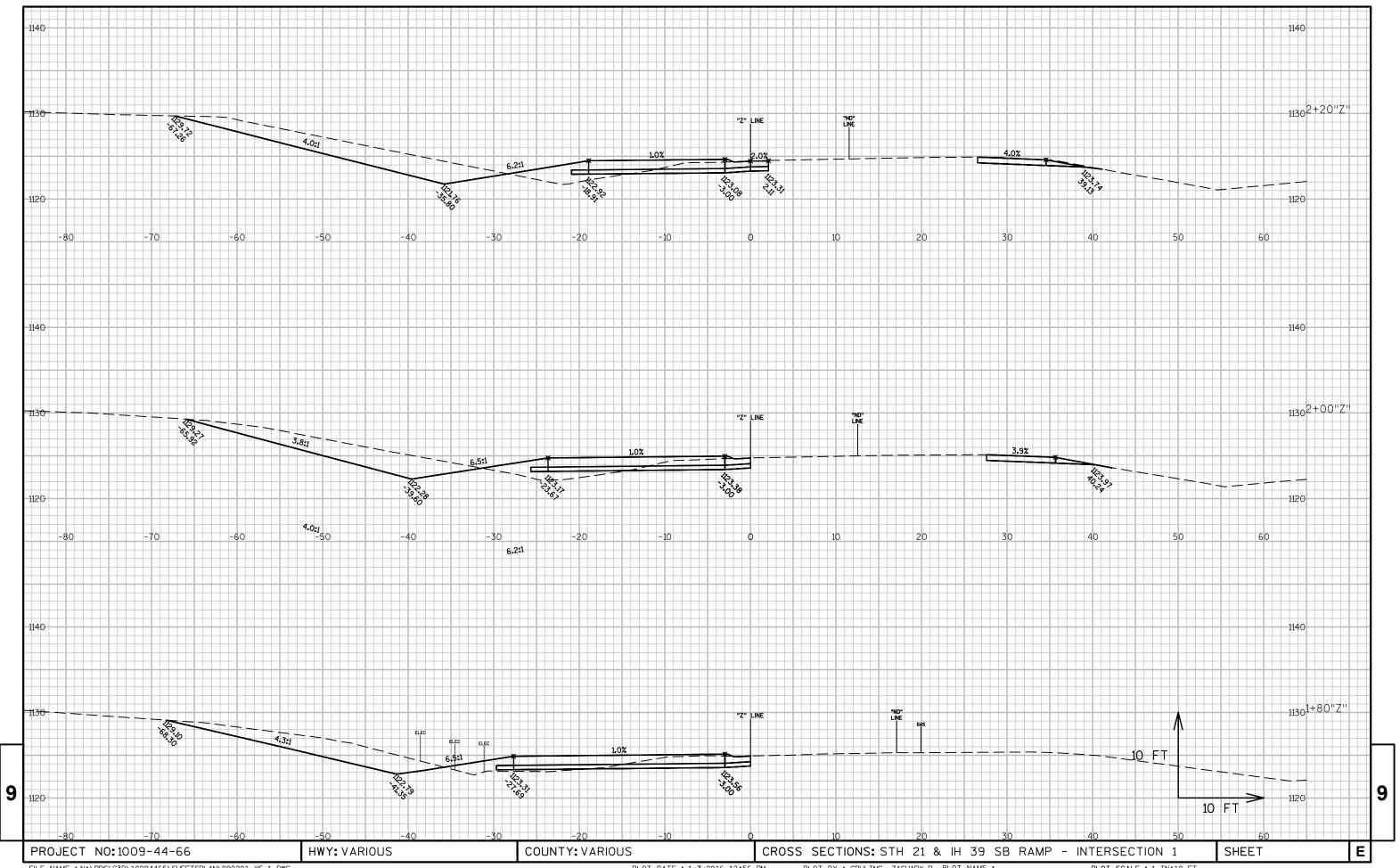
PROJECT NO:

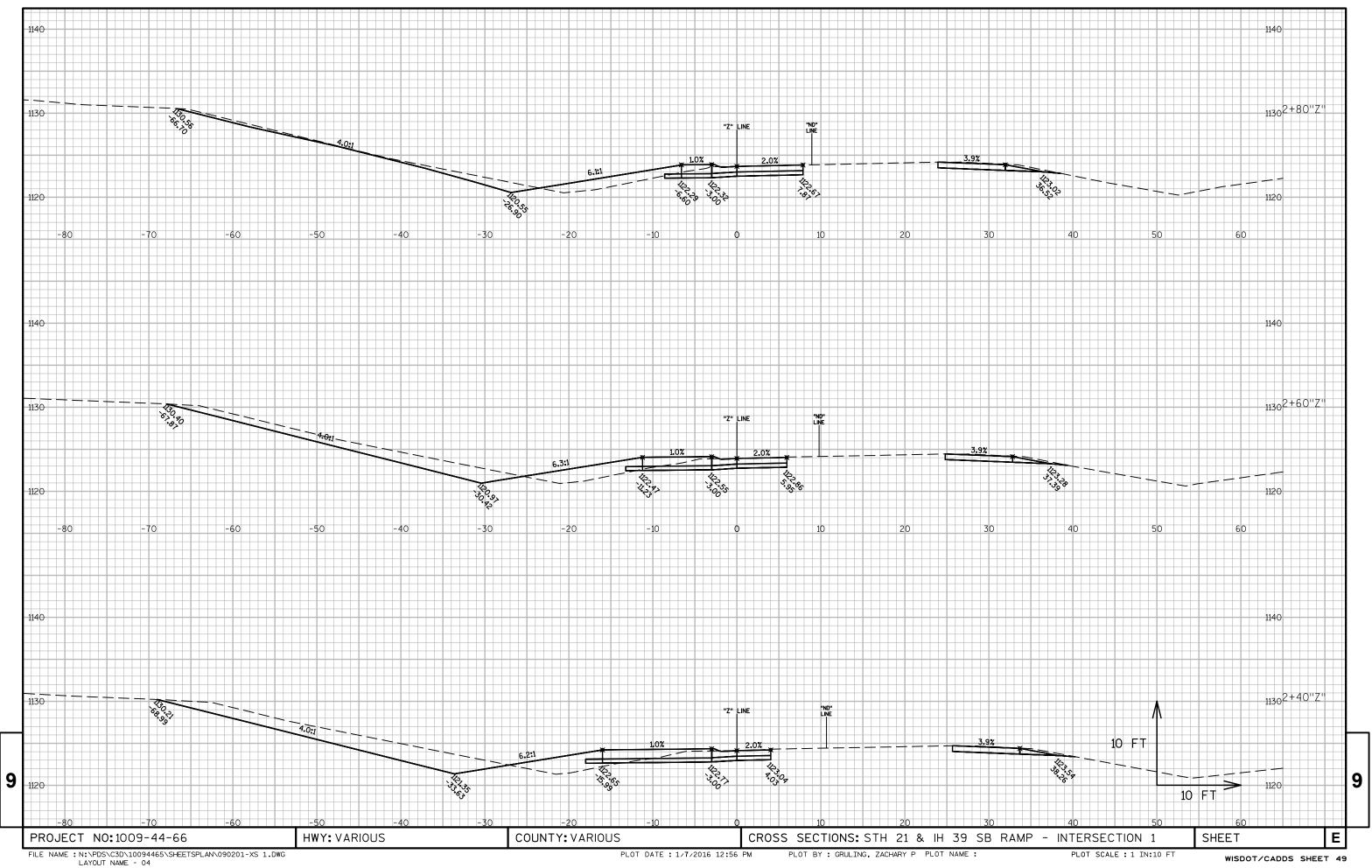
PLOT BY: mscj9h

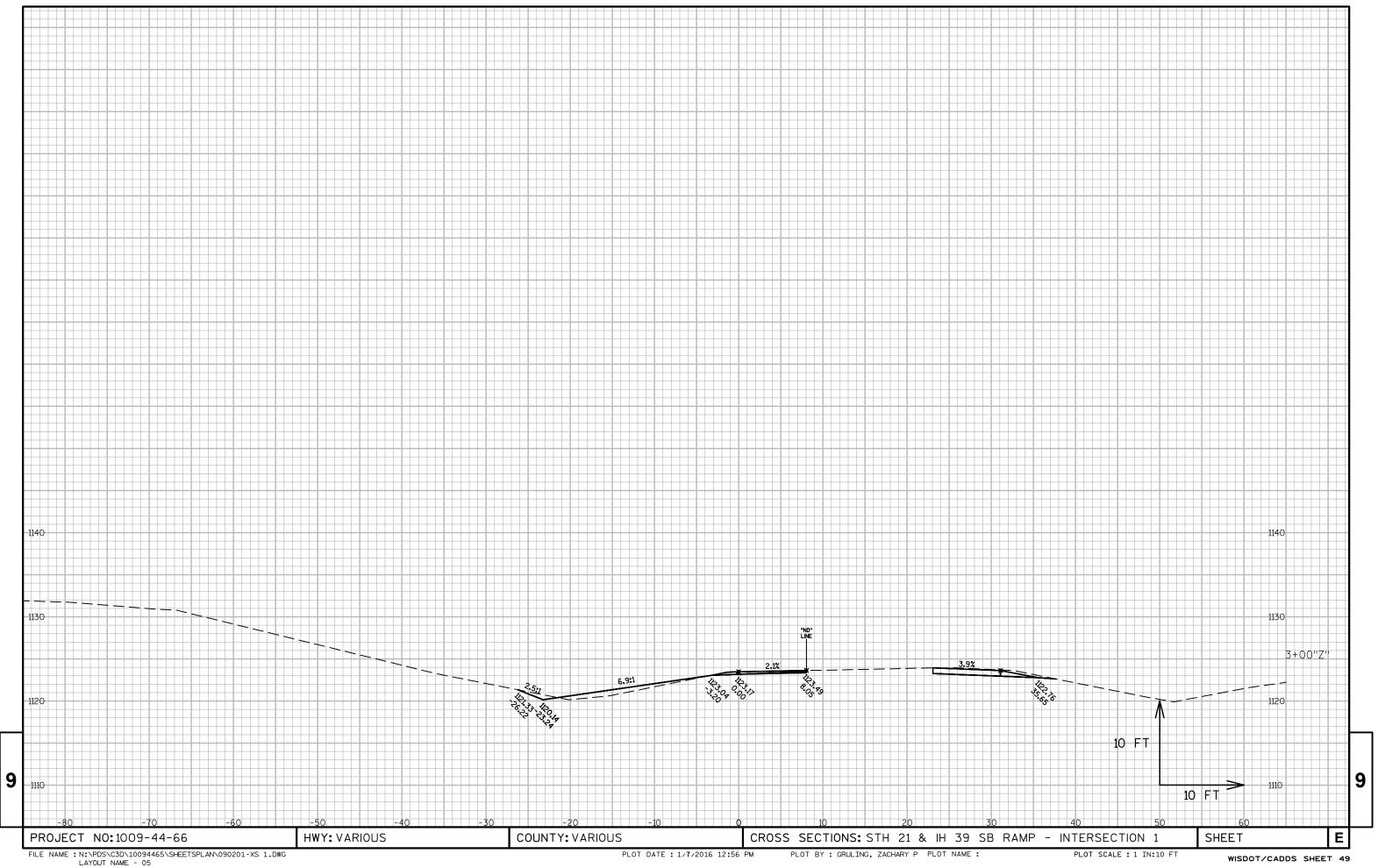
PLATE NO. W057-52.1

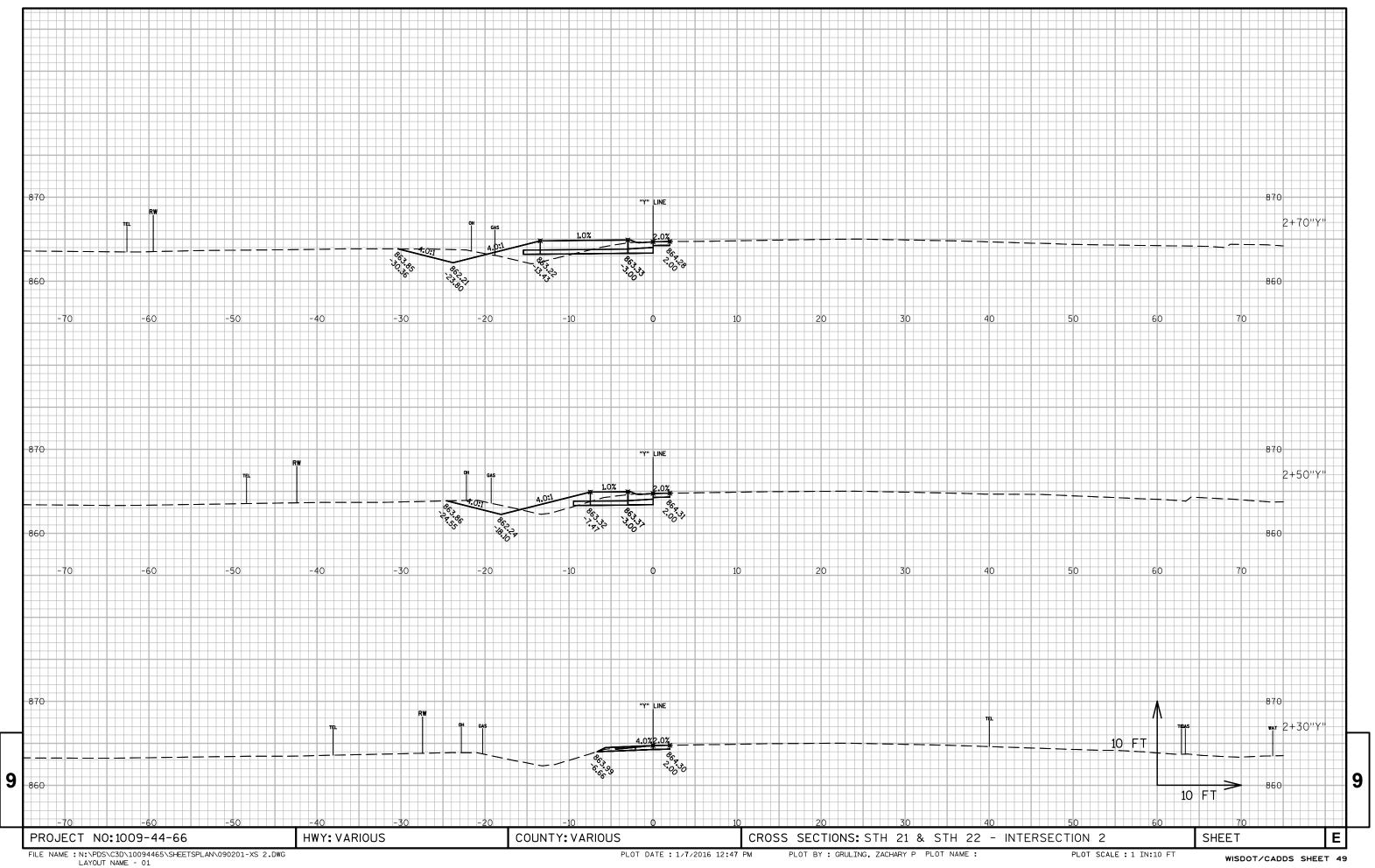


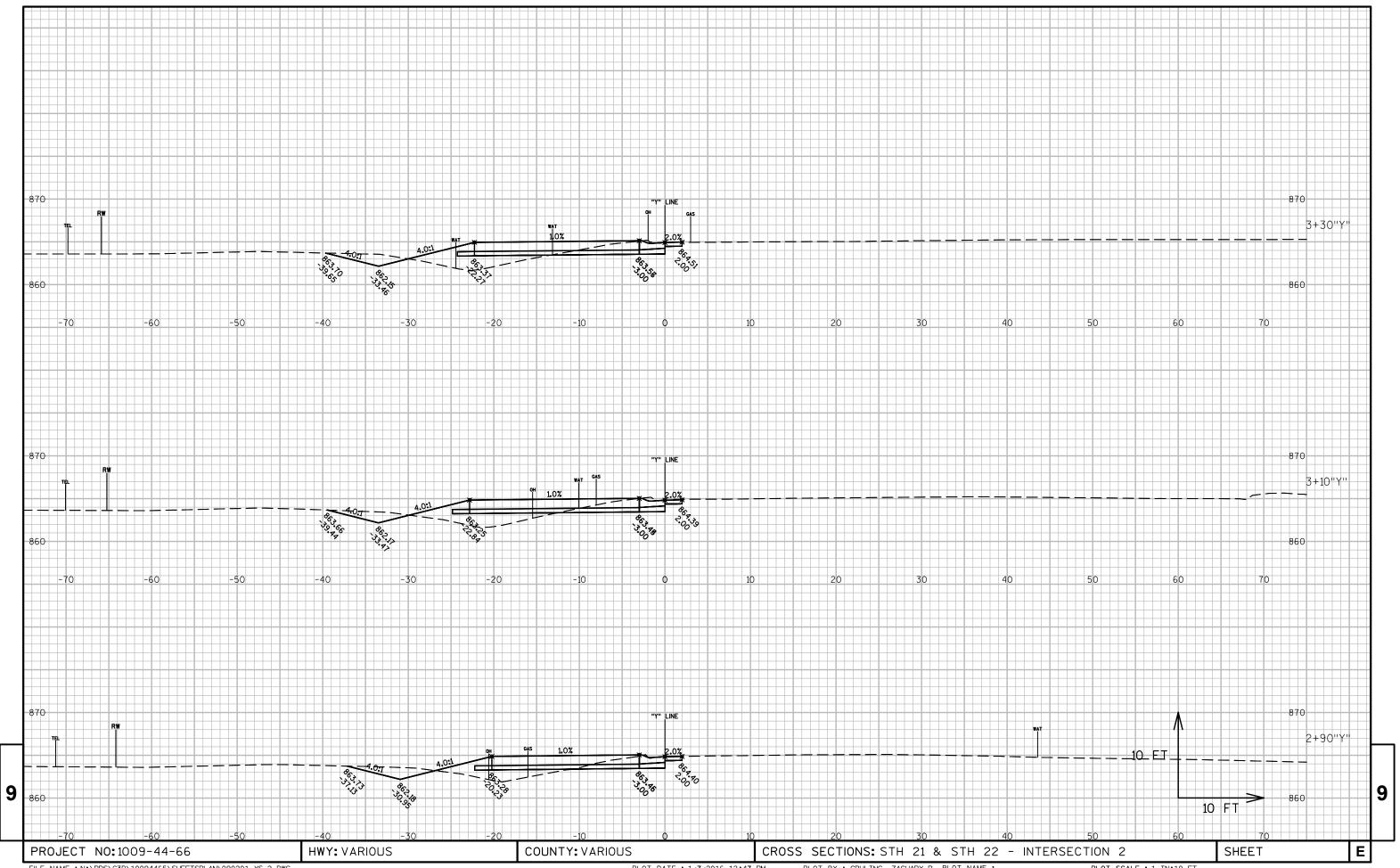


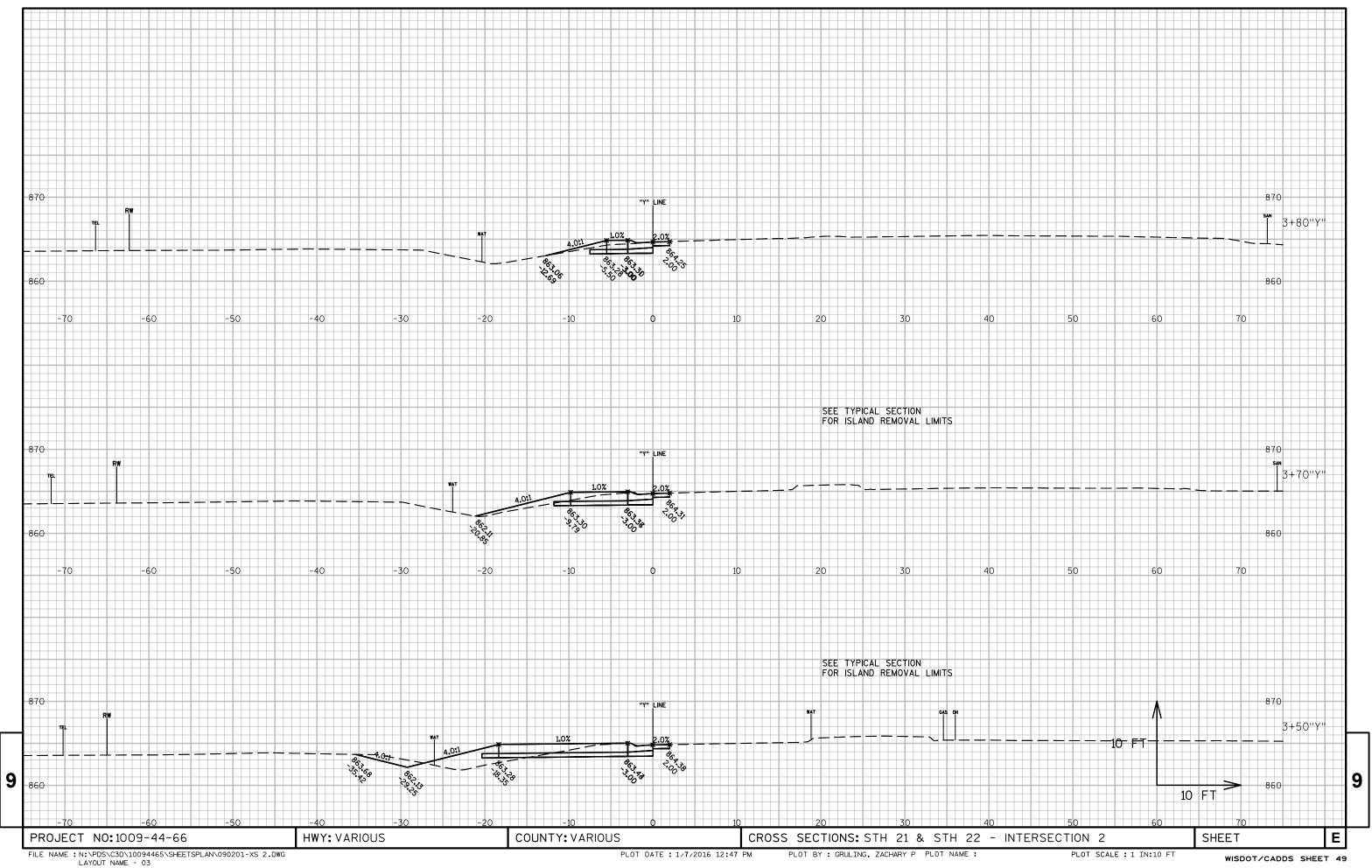


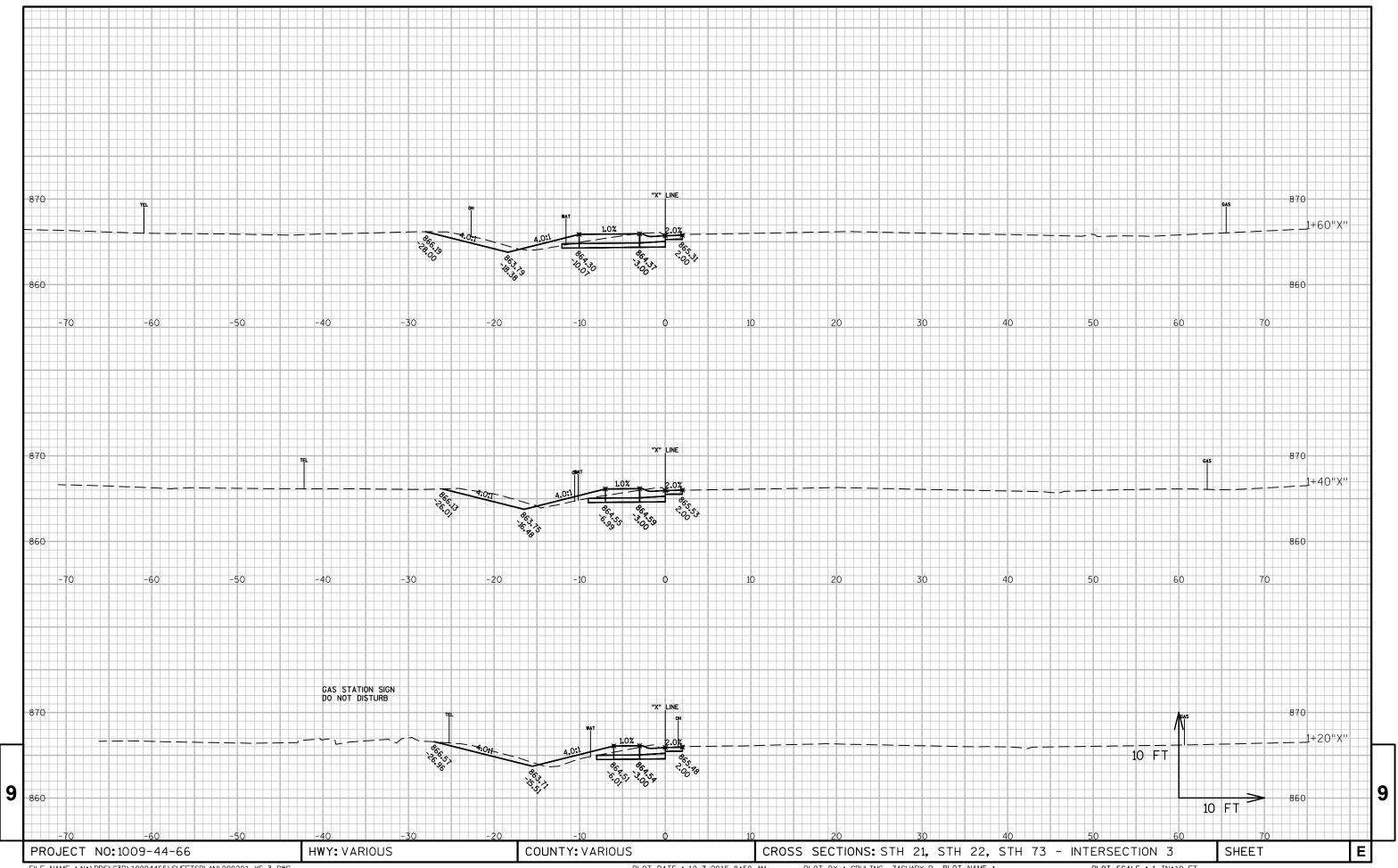


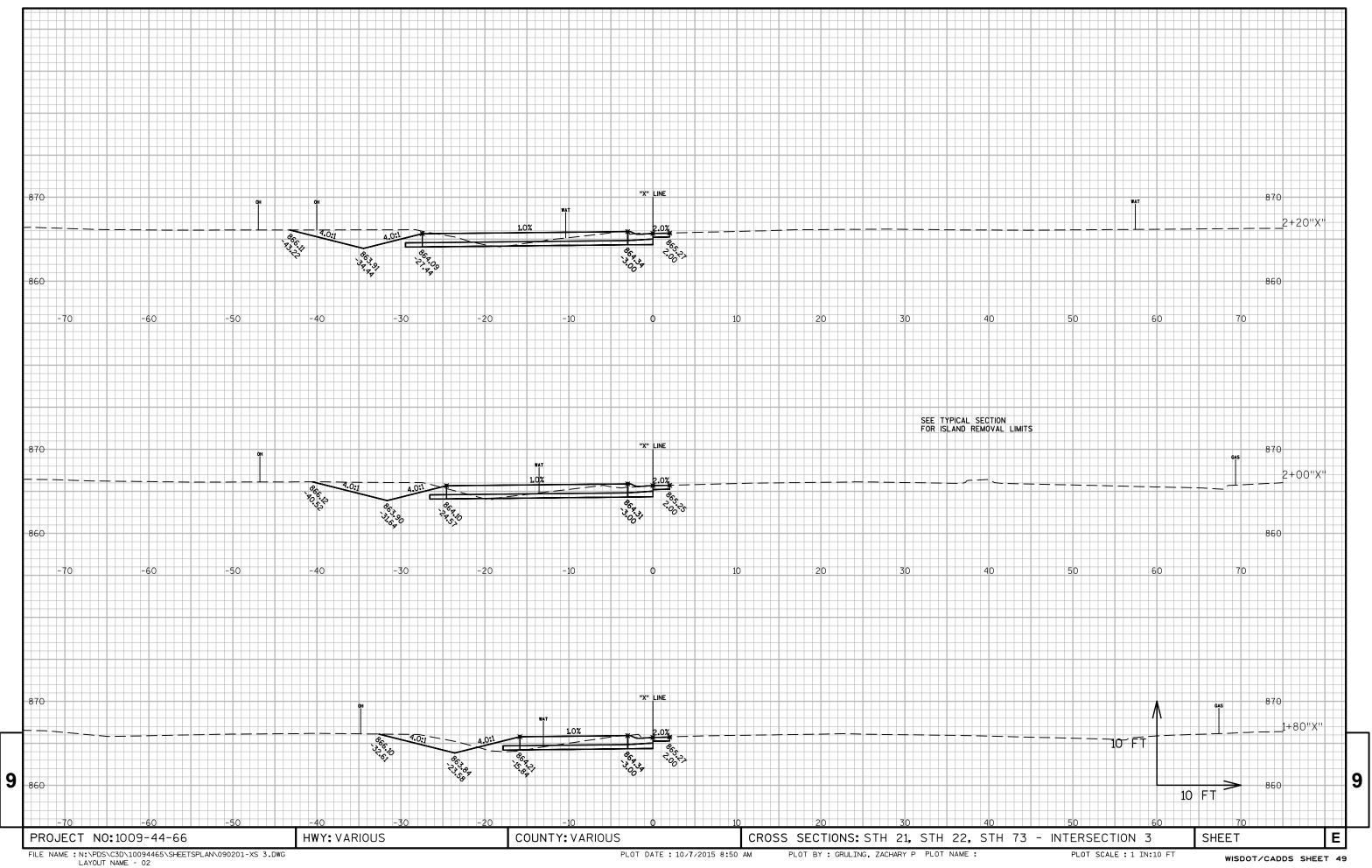


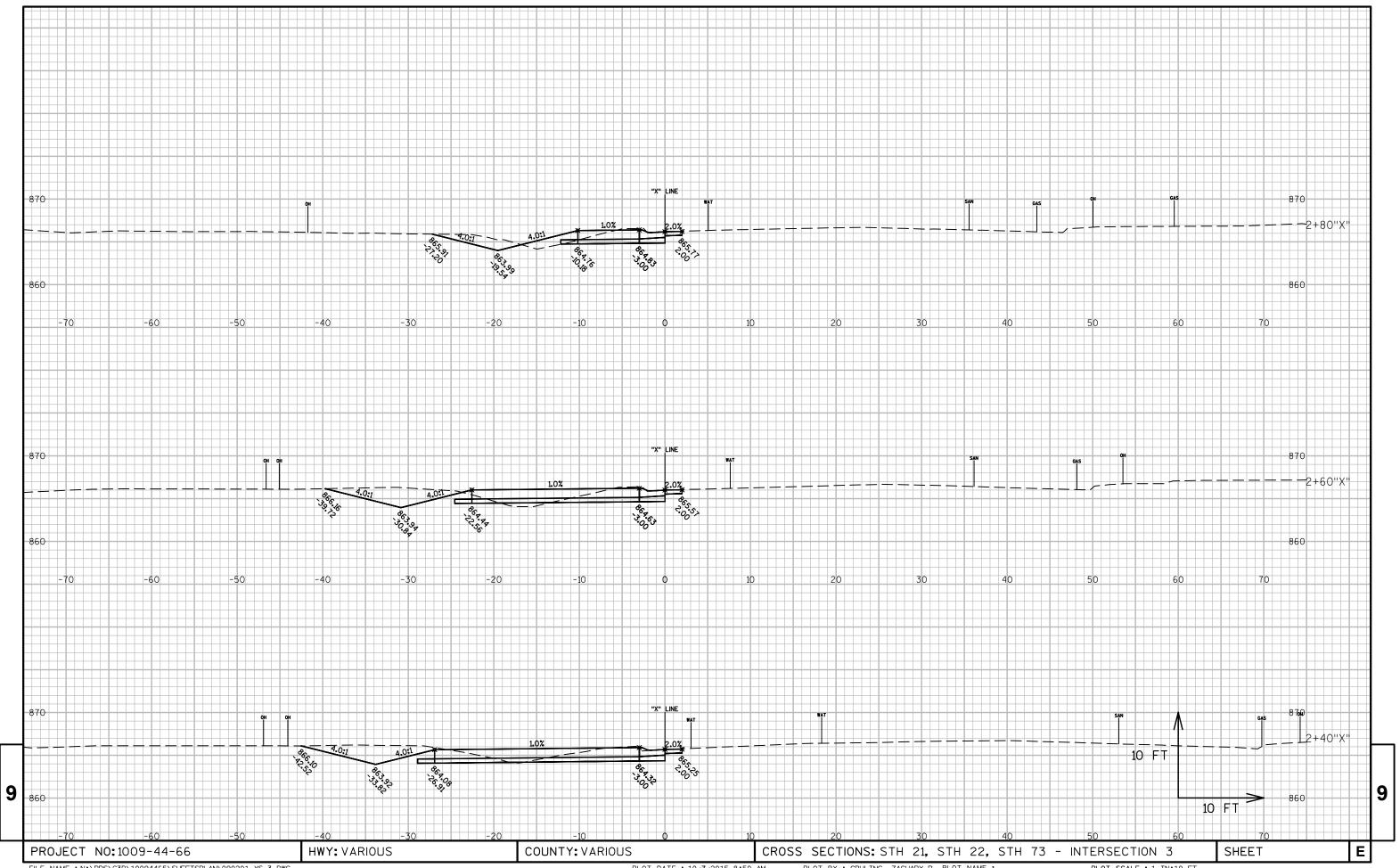


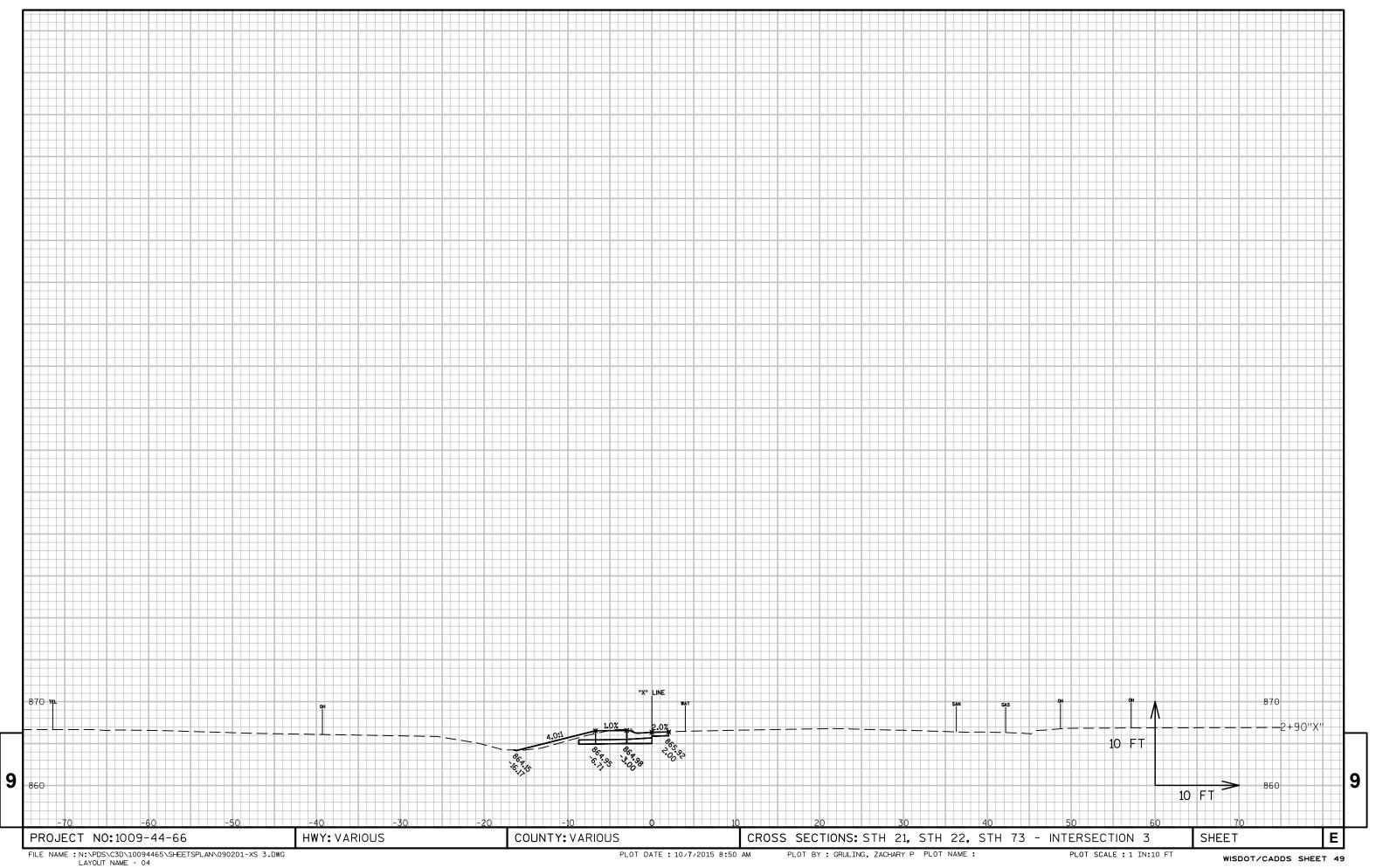


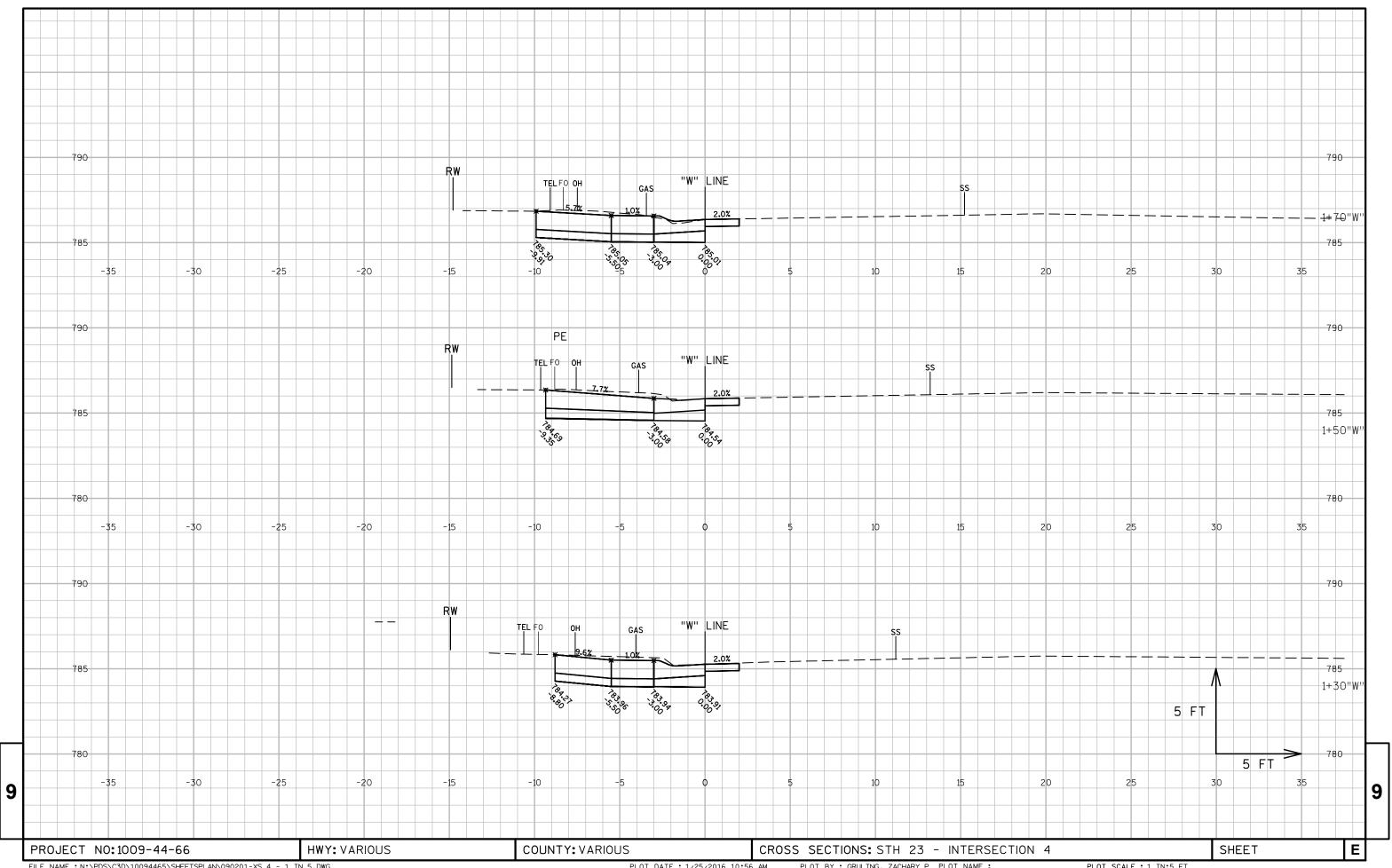


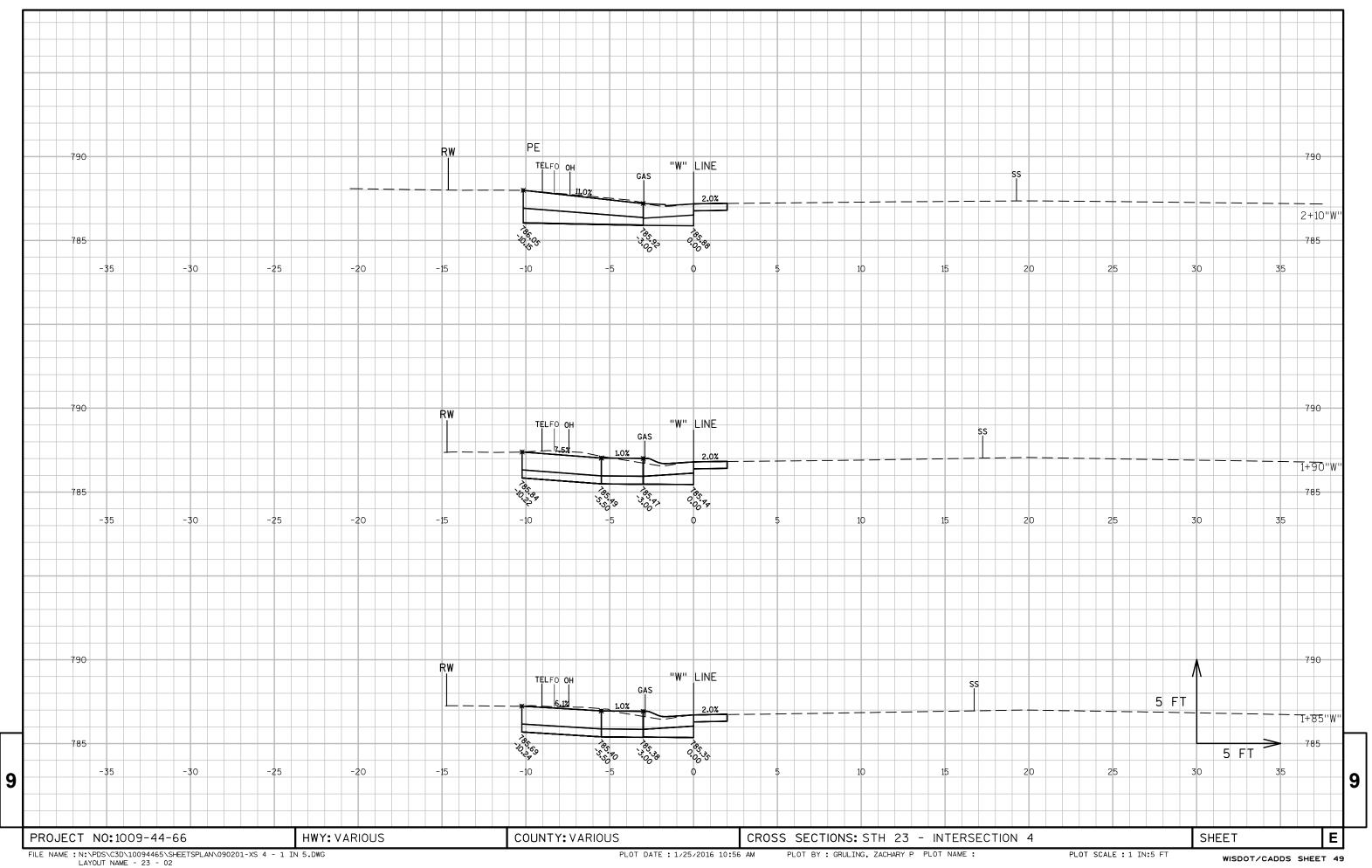


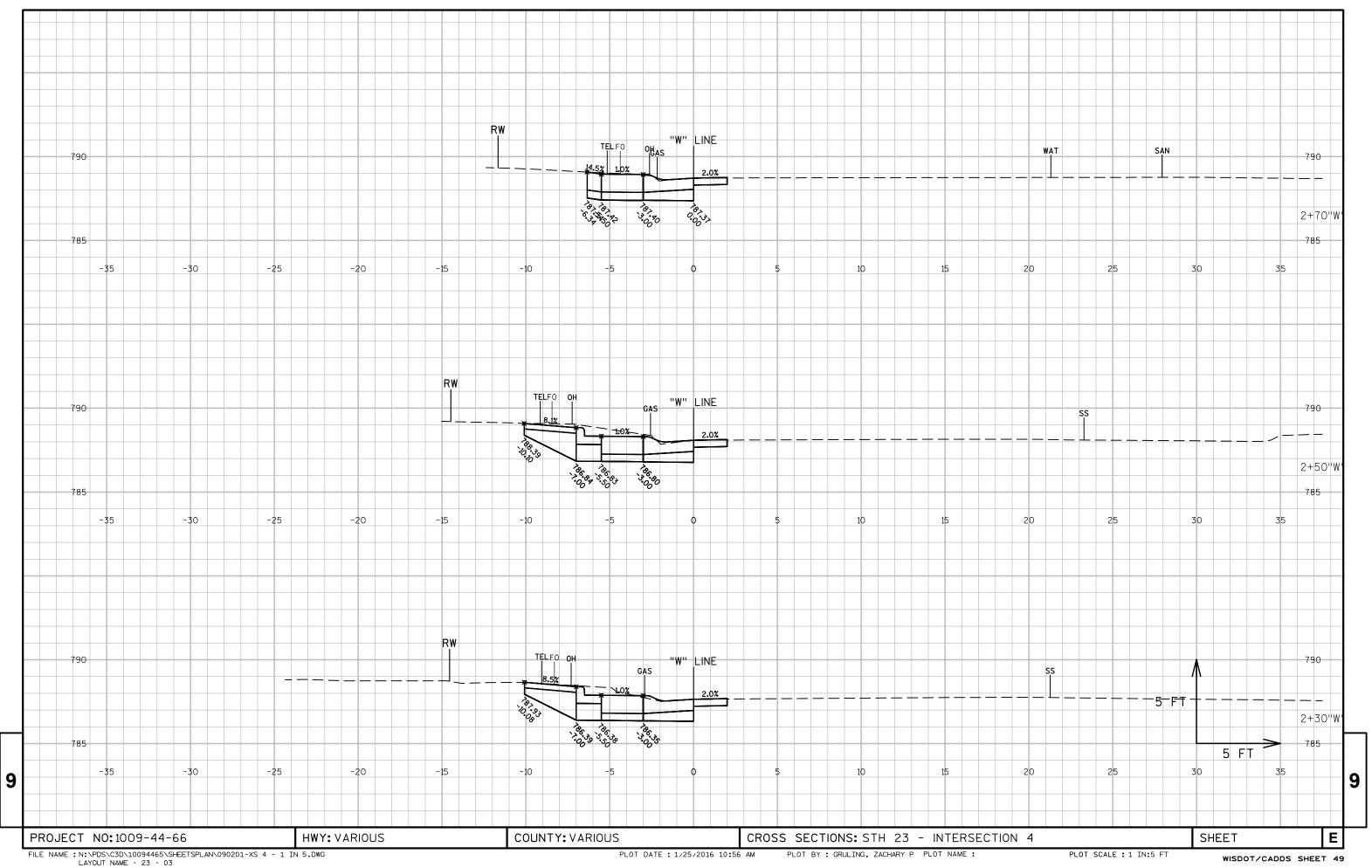


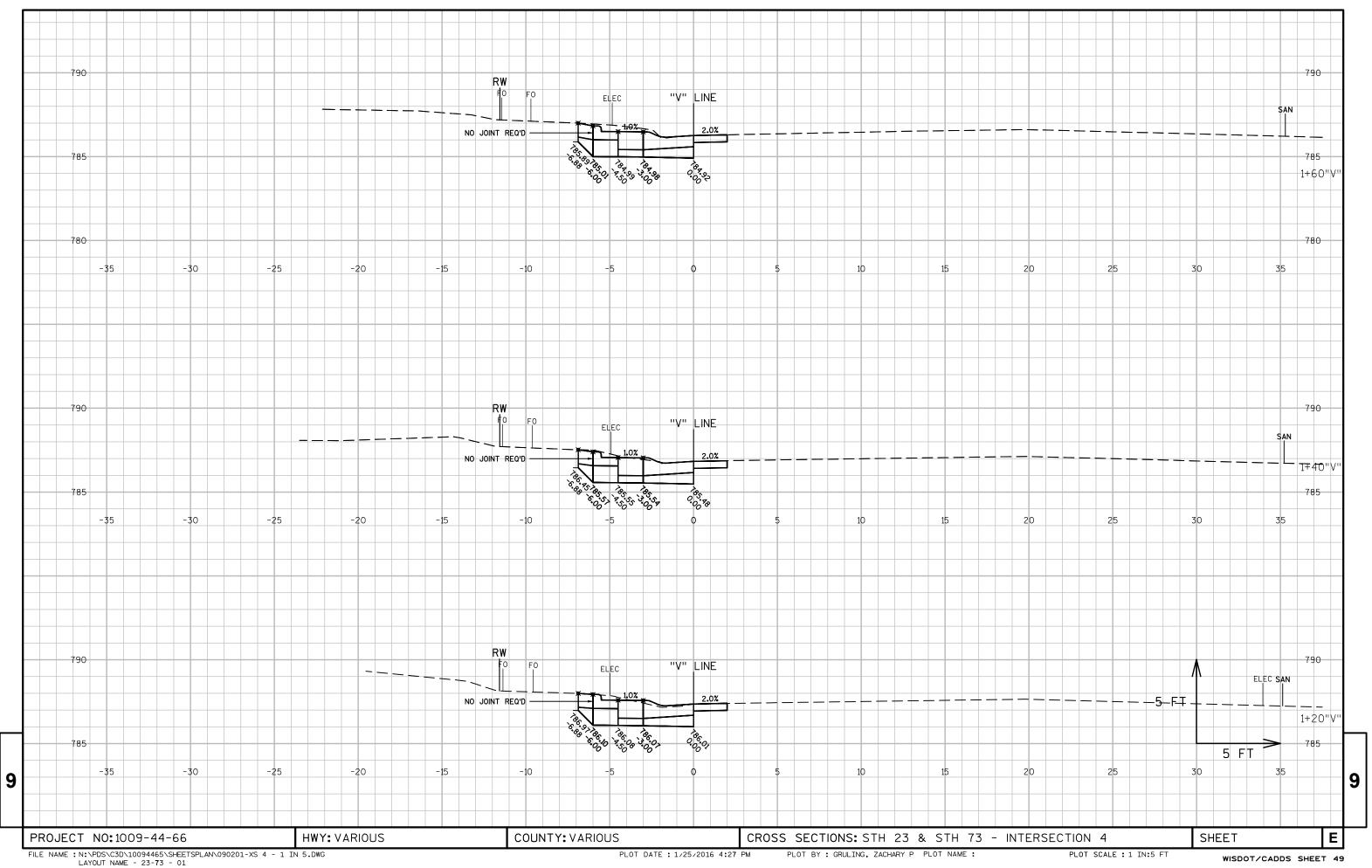


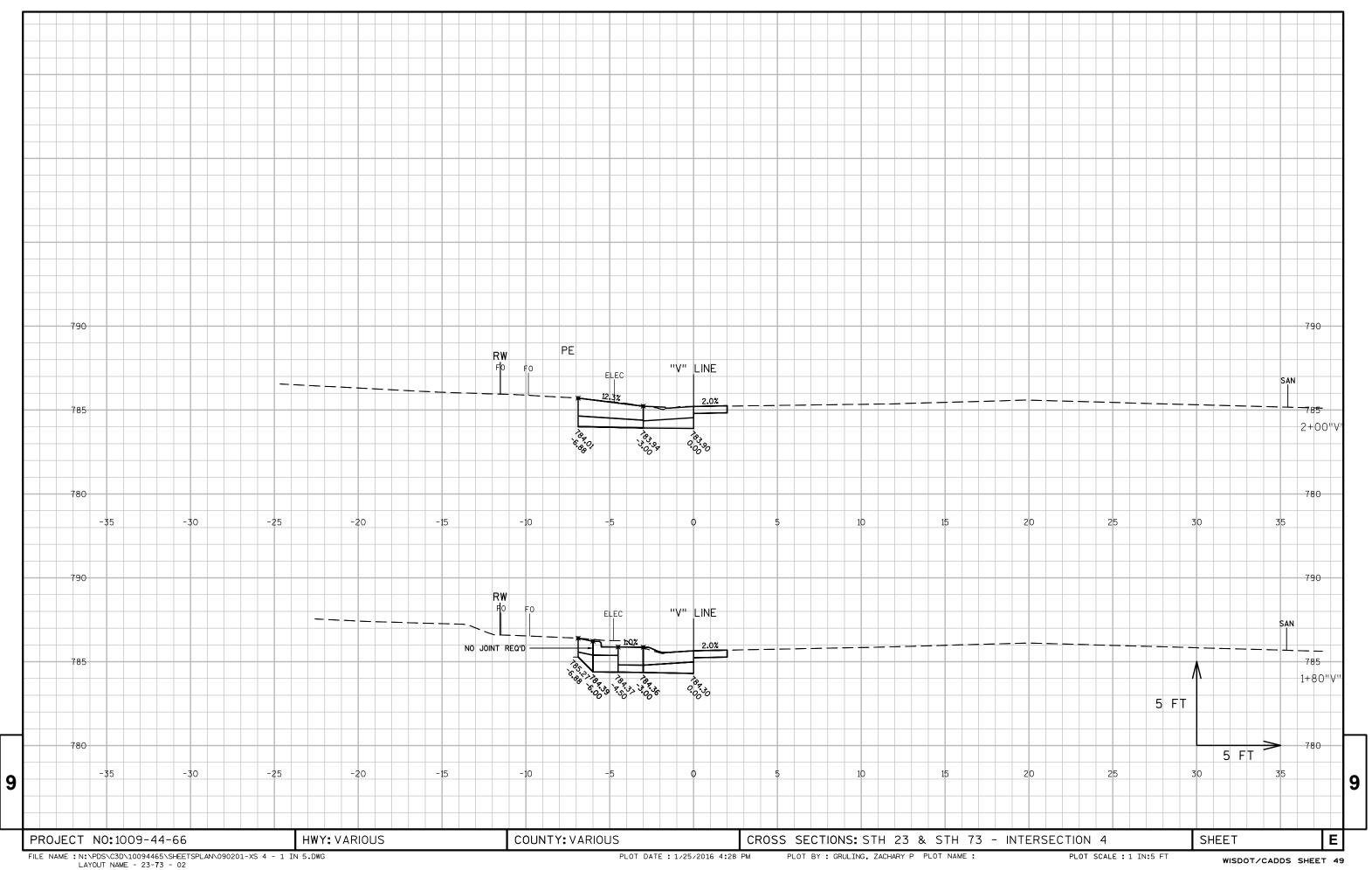














Wisconsin Department of Transportation

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