

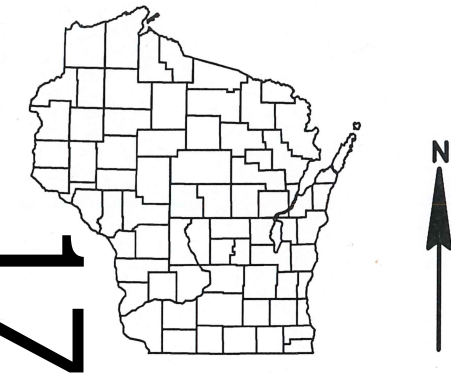
WKE
PROJECT ID: 2060-17-70
WITH:
COUNTY: RACINE

MAY 2016

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 106



DESIGN DESIGNATION

A.A.D.T.	2014	=	4,500 (CTH K - CTH G)	=	10,100 (CTH G - N. COUNTY LINE)
A.A.D.T.		=			
D.H.V.		=			
D.D.		=	58/42		
T.		=	6.9%		
DESIGN SPEED		=	55 MPH (35+40 - 262+35)		
		=	40 MPH (262+35 - 285+45)		
		=	55 MPH (285+45 - 430+64)		
ESALS		=			

CONVENTIONAL SYMBOLS		PROFILE	
PLAN		GRADE LINE	
CORPORATE LIMITS		ORIGINAL GROUND	
PROPERTY LINE		MARSH OR ROCK PROFILE (To be noted as such)	
LOT LINE		SPECIAL DITCH	
LIMITED HIGHWAY EASEMENT		GRADE ELEVATION	
EXISTING RIGHT OF WAY		CULVERT (Profile View)	
PROPOSED OR NEW R/W LINE		UTILITIES	
SLOPE INTERCEPT		ELECTRIC	
REFERENCE LINE		FIBER OPTIC	
EXISTING CULVERT		GAS	
PROPOSED CULVERT (Box or Pipe)		SANITARY SEWER	
COMBUSTIBLE FLUIDS		STORM SEWER	
MARSH AREA		TELEPHONE	
WOODED OR SHRUB AREA		WATER	
		UTILITY PEDESTAL	
		POWER POLE	
		TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

STH 38

CTH K TO NORTH COUNTY LINE

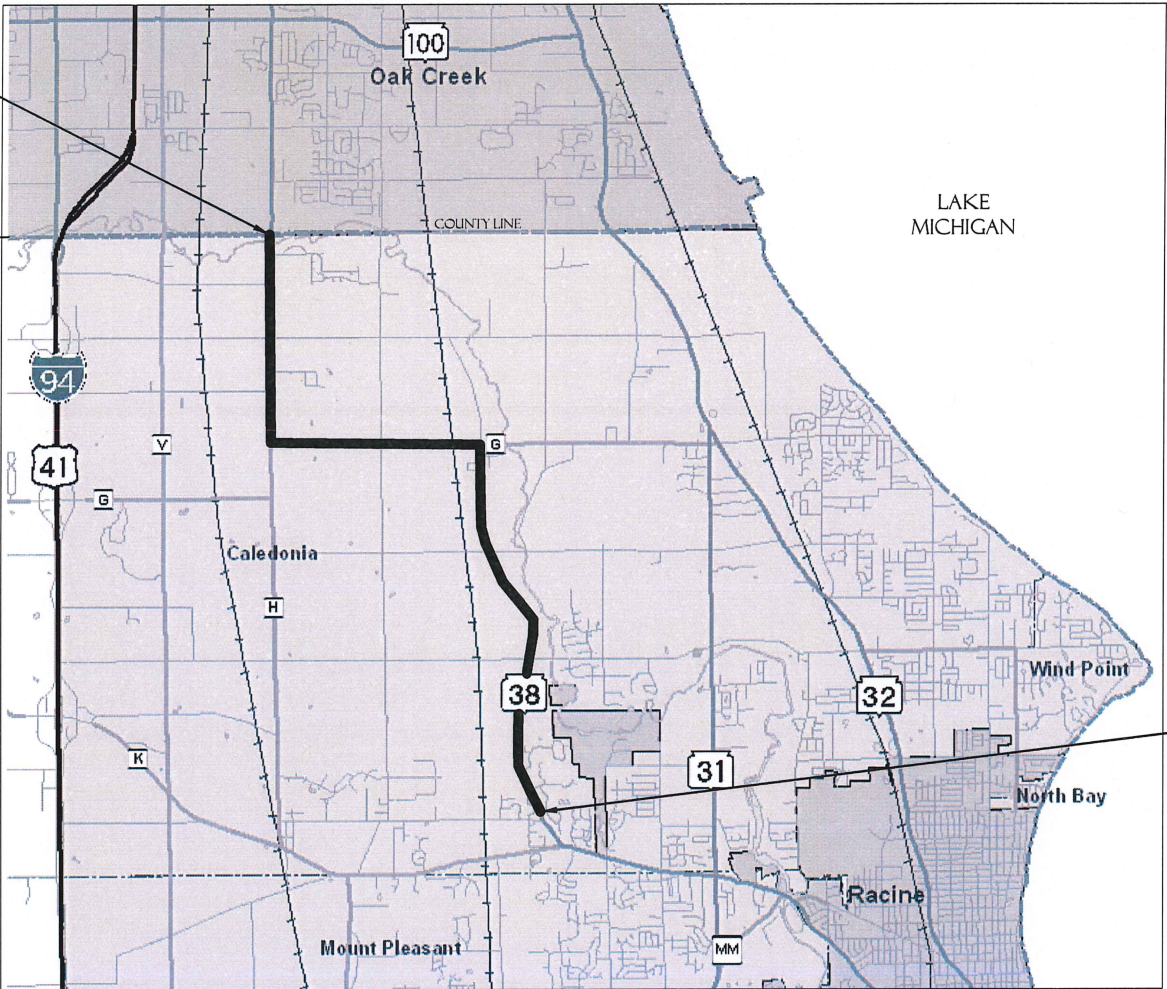
STH 38
RACINE COUNTY

STATE PROJECT NUMBER
2060-17-70

R-22-E

END PROJECT
STA 430+63

MILWAUKEE
COUNTY
COUNTY LINE
RACINE
COUNTY



T-4-N

BEGIN PROJECT
STA 35+40
Y = 199,489.77
X = 617,173.16

LAYOUT
SCALE 0 2 MILE

TOTAL NET LENGTH OF CENTERLINE = 7.5 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, RACINE COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
2060-17-70	WISC 2016168	1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WISDOT
Designer	MARTIN VILLACA
Project Manager	HEATHER SACKMAN
Regional Examiner	
Regional Supervisor	REEM SHAHIN

APPROVED FOR THE DEPARTMENT
DATE: 2/18/16 *Heather Sackman*
(Signature)

E

GENERAL NOTES

LOCATIONS SHOWN ON THIS PLAN ARE APPROXIMATE ONLY, AND ARE TO BE VERIFIED IN THE FIELD BY THE ENGINEER. RECENTLY CONSTRUCTED DRIVEWAYS OR OTHER FEATURES MAY NOT APPEAR ON THIS PLAN.

THE LOCATIONS OF UTILITIES NOTED OR SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS' HOTLINE AND A CALL TO UTILITIES THAT HAVE A FACILITY WITHIN THE PROJECT AREA. ANY UTILITY WHICH IS NOT A MEMBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

CONTRACTOR TO PROTECT ALL SURVEY MONUMENTS AND KEEP CONSTRUCTION EQUIPMENT AT LEAST 2 FEET AWAY FROM MONUMENTS.

CONTRACTOR TO PROTECT HMOD CALEDONIA GPS AND KEEP CONSTRUCTION EQUIPMENT AT LEAST 10 FEET AWAY FROM HMOD CALEDONIA GPS. ENSURE THAT HMOD CALEDONIA GPS IS NOT DISTURBED, BUMPED INTO OR MOVED DURING THE DURATION OF THE PROJECT. NOTIFY JACOB ROCKWEILER IMMEDIATELY IF HMOD CALEDONIA GPS IS DISTURBED, BUMPED INTO OR MOVED DURING THE DURING CONSTRUCTION OPERATIONS.

ALL SIGN LOCATIONS SHALL BE REVIEWED BY THE ENGINEER PRIOR TO INSTALLATION.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

TRAFFIC CONTROL DRUMS OR OTHER ENGINEER-APPROVED DEVICE SHALL BE PAID AS TRAFFIC CONTROL DRUMS.

MAINTAIN COMMERCIAL AND RESIDENTIAL ACCESS THROUGHOUT THE DURATION OF THE PROJECT.

WisDOT PROJECT MANAGER

Heather Sackman
WisDOT Project Manager SE-Region
141 NW Barstow St
Waukesha, WI 53187
Phone: (262) 521-5376
Fax: (262) 548-8645
heather.sackman@dot.wi.gov

WisDOT Maintenance

Gail Lantz
Racine County - Coordinator
141 NW Barstow St
PO Box 798
Waukesha, WI 53187
Phone: (414) 750-0994
gail.lantz@dot.wi.gov

WDNR LIASON FOR RACINE COUNTY

Craig Webster
WDNR Service Center
141 NW Barstow Street
Waukesha, WI 53188
Phone: (262) 574-2141
Cell Phone: (414) 303-3011
Craig.Webster@Wi.Gov

WisDOT Signals

WisDOT Electrical Field Unit
935 S. 60th Street
West Allis, WI 53214
Phone: (414) 266-1170

Elizabeth Lloyd-Weis, Signal Ops Lead
WisDOT Traffic Signal Operations
141 NW Barstow Street
Waukesha, WI 53187
Phone: (414) 750-2605

WisDOT HEIGHT MODERNIZATION (HMOD) PROGRAM MANAGER

JACOB ROCKWEILER, P.E.
Phone: (608) 516-6362
jacob.rockweiler@dot.wi.gov.

ORDER OF SECTION 2 DETAIL SHEETS

- GENERAL NOTES & CONTACTS
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- TRAFFIC CONTROL DETAILS
- PAVEMENT MARKING
- ALIGNMENT LAYOUT

HMA PAVEMENT NOTES

LONGITUDINAL JOINT REPAIR: HMA PAVEMENT TYPE 4 MT 58-28 S

2 1/2 - INCH 12.5 MM

STA 35+40 PAVEMENT REPAIR: HMA PAVEMENT TYPE 4 MT 58-28 S

2 - INCH UPPER LAYER 12.5 MM
3 - INCH LOWER LAYER 12.5 MM

UTILITIES AND CONTACTS

LaTroy Brumfield, Project Manager
We Energies (Electric)
333 W. Everett St - A299
Milwaukee, WI 53203
Phone: (414) 221-5617
Fax: (414) 221-2336
latroy.brumfield@we-energies.com

Mike Olsen
ATC Management, Inc.
801 O'Keefe Road
PO Box 6113
DePere, WI 54115
Phone: (920) 338-6582
molsen@atcllc.com

LaTroy Brumfield, Project Manager
We Energies (Gas Operations)
333 W. Everett St - A299
Milwaukee, WI 53203
Phone: (414) 221-5617
Fax: (414) 221-2336
latroy.brumfield@we-energies.com

Mark Eder
AT&T Wisconsin - Communication Line
2005 Pewaukee Rd
Waukesha, WI 53188-2443
(262) 896-7434
me1754@att.com

Kevin Huff
Centurylink/Qwest National Network
13057 S. Monitor Avenue
Palos Heights, IL 60643
Phone: (708) 837-7927
Fax: (708) 371-8674
kevin.huff@centurylink.com

Michael Johnson, Senior Network Specialist
TDS Metrocom
16924 West Victor Road
New Berlin, WI 53151
Phone: (262) 754-3052
Fax: (262) 717-6002
michael.johnson@tdstelecom.com

Richard Trgovec, OSP Engineer
Midwest Fiber Networks
6070 North Flint Road
Glendale, WI 53209
Phone: (414) 672-5612
Fax: (414) 226-2269
rtgoovec@midwestfibernetworks.com

Debbie Saddler, Project Manager
Teleport Communications America, LLC
6070 N. Flint Rd.
c/o Northwind Technical Services
Glendale, WI 53209
Phone: (414) 459-3572
d.saddler@northwindtech.com

Jim Kostuch, Supervisor - OSP Network
PAETEC Business Services
13935 Bishops Dr.
Brookfield, WI 53005
Phone: (262) 792-7938
james.kostuch@windstream.com

Jim Kostuch, Project Manager
Windstream KDL, Inc.
(f/k/a KDL Telecom)
13935 Bishop's Dr.
Brookfield, WI 53005
Phone: (262) 792-7938
james.kostuch@windstream.com

UTILITIES AND CONTACTS - (continued)

Jim Burton, Facilities Engineer II/OSP-East
Sprint Communications, L.P.
5600 N. River Road, STE 200
Rosemont, IL 60018
Phone (708) 955-6659
Fax: (630) 379-5958
james.m.burton@sprint.com

Carl Donohue
AT&T Corp
866 Rock Creek Road
Plano, IL 60545
Phone: (847) 420-9115
Fax: (847) 244-9597
cd8729@att.com

TW Cable Engineering
Time Warner Cable
1320 N Dr. Martin Luther King Dr.
Milwaukee, WI 53212
Phone: (414) 277-4045
wis.engineering@twcable.com

Steven Cramer
Time Warner Cable
1320 N Dr Martin Luther King Jr Dr
Milwaukee, WI 53212-4002
(414) 277-4045
wis.engineering@twcable.com

James Burton
Sprint Communications Co LP
400 West Grand Ave
Elmhurst, IL 60126
(630) 379-5958
james.burton@ericsson.com

OSP National Support/Investigations
Verizon Business Division
2400 N. Glenville
Richardson, TX 75082
Phone: (972) 729-6322
Investigations@Verizon.com

Network Relocations
Level 3 Communications
1025 Eldorado Blvd
Broomfield, CO 80021
Phone: (720) 888-0336
level3.networkrelocations@level3.com

Robert Church
Frontier Communications of WI LLC
2222 West Wisconsin Street
Portage, WI 53901
(608) 742-1817
robert.church@ftr.com

Brahim Gaddour
TW Telecom
3235 Intertech Drive, Suite 600
Brookfield, WI 53045
Phone: (414) 908-1027
relo@level3.com

CenturyLink - Communication Line
10 E Doty St, Ste 800
Madison, WI 53703

DIGGERS HOTLINE

Dial **811** or (800) 242-8511

www.DiggersHotline.com

UTILITIES & CONTACTS - LOCAL GOVERNMENT

Keith Haas
City of Racine - Sewer
800 Center St.
Racine, WI 53403
(262) 636-9434
keith.haas@cityofracine.org

Ron Pritzlaff
City of Oak Creek Sewer & Water Utility
170 West Drexel Ave
Oak Creek, WI 53154
(414) 570-8200
rpritzlaff@water.oak-creek.wi.us

Brian Johnston
City of Oak Creek (Road & Lighting)
8640 S Howell Avenue
Oak Creek, WI 53154
(414) 768-6531
bjohnston@oakcreekwi.org

Debra Jensen, Planning Services Sup.
Milwaukee Metro Sewerage District
260 W Seeboth St
Milwaukee, WI 53204
Phone: (414) 225-2143
djensen@mmsd.com

Tony Bunkelman
Caledonia Storm Sewer Util. Com. - Steam
6922 Nicholson Rd
Caledonia, WI 53108
(262) 835-6416
abunkelman@caledoniawi.com

Robert Pucely
Village of Mount Pleasant - Water & Sewer
8811 Campus Dr
Mount Pleasant, WI 53406
(262) 664-7847
rpucely@mtpleasantwi.gov

Tony Beyer
Mount Pleasant Storm Water Dist. 1 - Sewer
8811 Campas Dr
Mount Pleasant, WI 53406
(262) 664-7849
tbeyer@mtpleasantwi.gov

Gregory Heisel
Milwaukee County DPW (Road & Lighting)
10320 W. Watertown Plank Rd.
Milwaukee, WI 53226
(414) 257-6566
Greg.Heisel@milwcnty.com

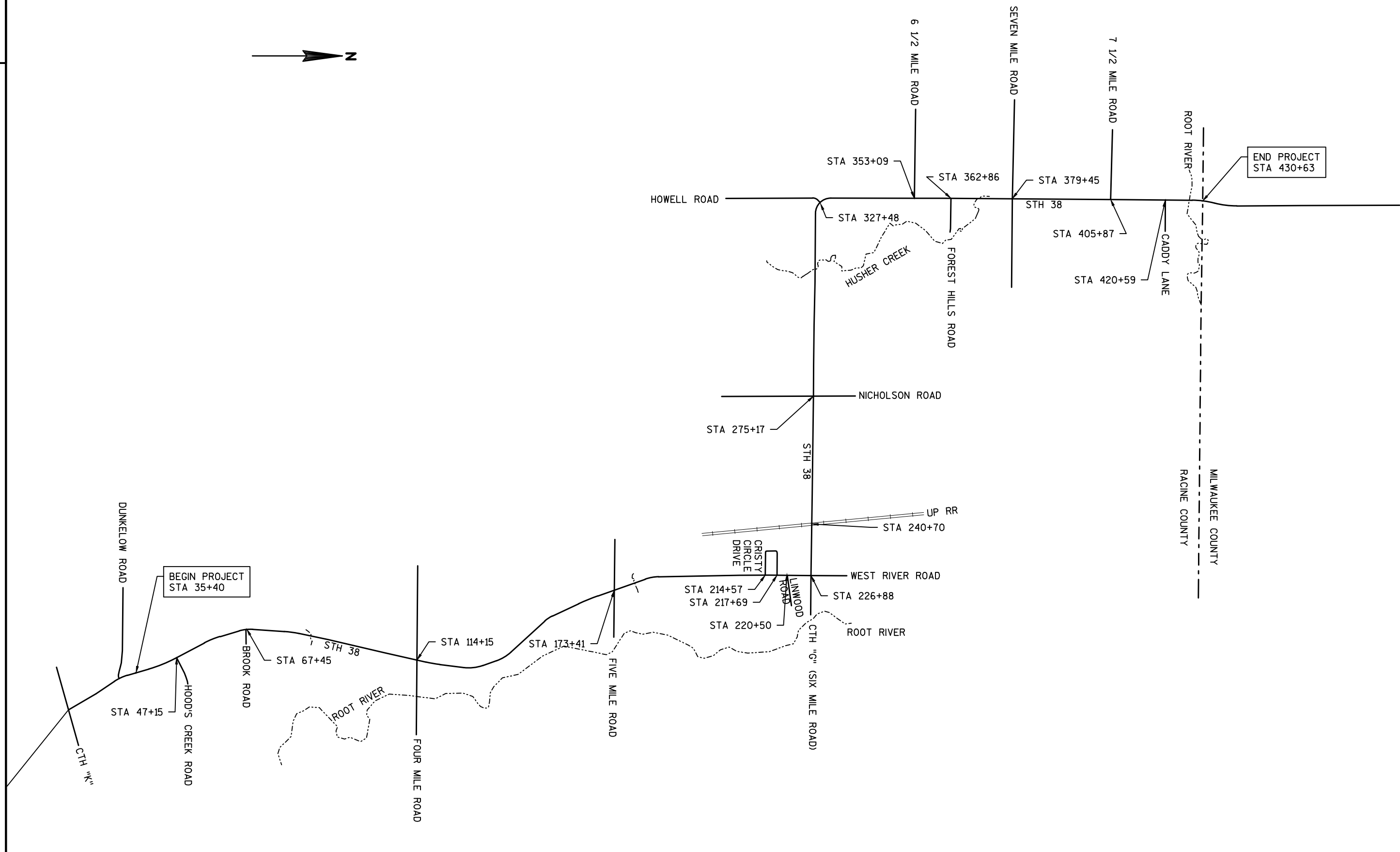
Robert J. Lui, Utility Manager
Village of Caledonia - Water & Sewer
6922 Nicholson Rd
Caledonia, WI 53108
(262) 681-3900

Andrew Kothe
Caddy Vista San. Dist. - Water & Sewer
10201 Caddy Ln
Caledonia, WI 53108
(414) 762-7878

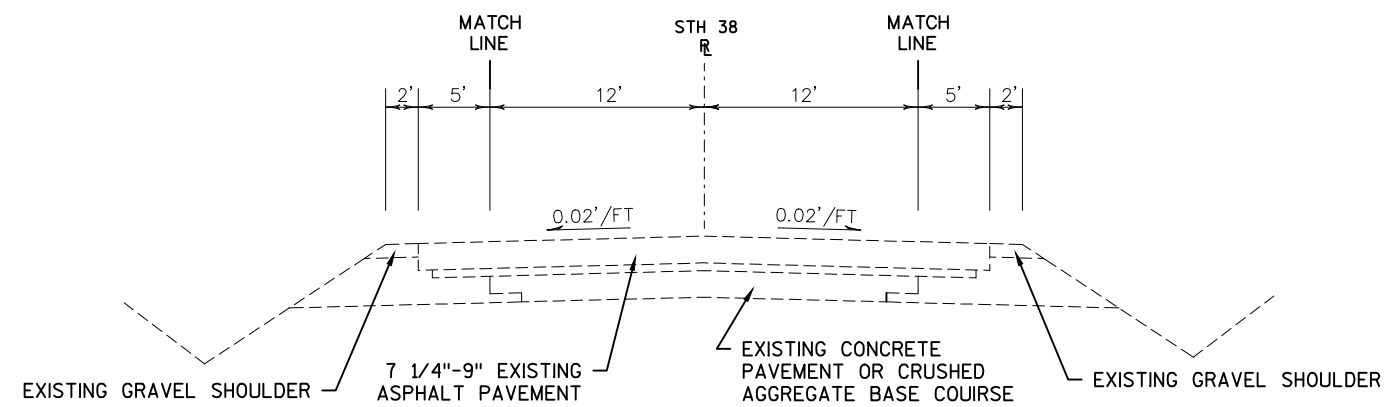
Jeff Fidler
City of Racine (Road & Lighting)
800 S. Marquette St.
Racine, WI 53403
(262) 636-9126

RAILROAD

John Venice, Manager of Special Projects
Union Pacific Railroad Company
101 North Wacker Drive, Suite 1920
Chicago, IL 60606
Phone: (312) 777-2043
Fax: (402) 233-2769
jvenice@up.com

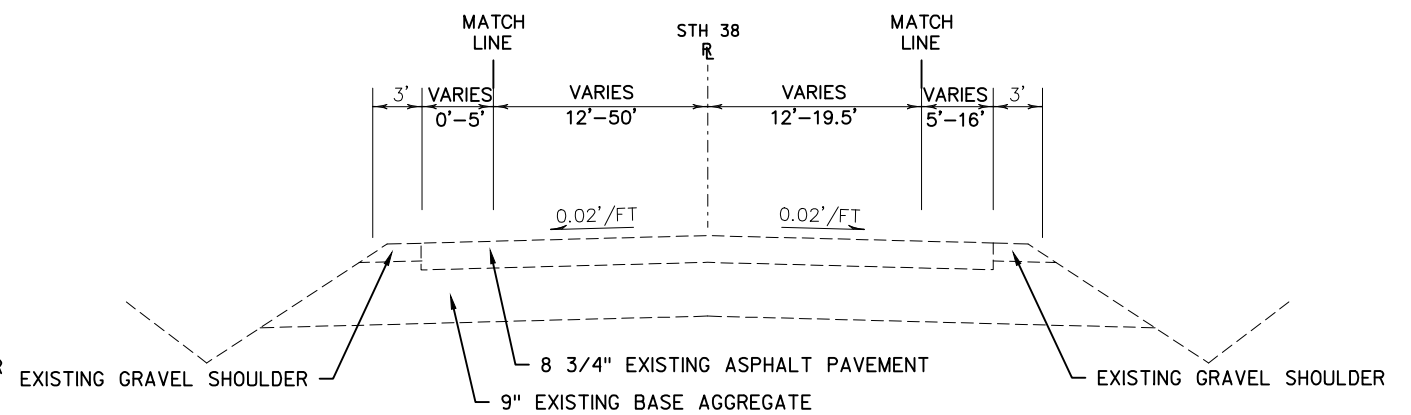


PROJECT NO:2060-17-70	HWY:STH 38	COUNTY:RACINE	PROJECT OVERVIEW	SHEET	E
-----------------------	------------	---------------	------------------	-------	---



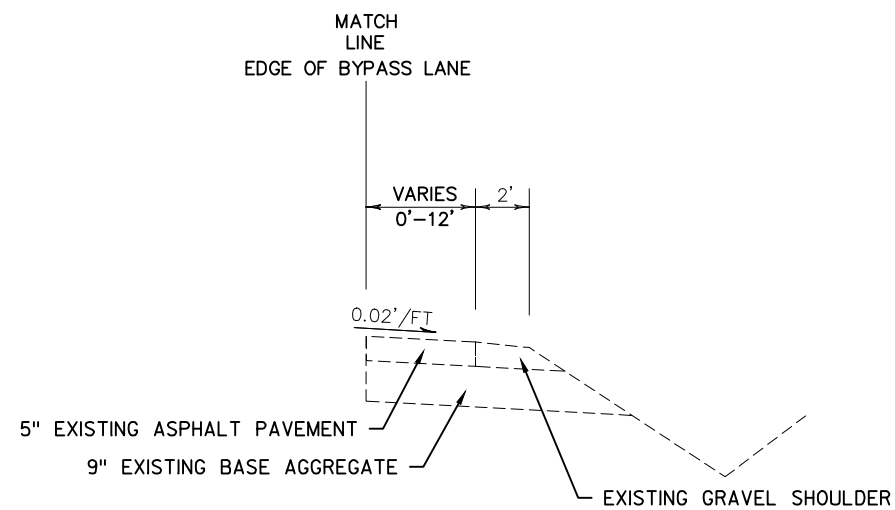
EXISTING TYPICAL SECTION
STH 38

STA 35+40 - STA 108+00
STA 124+00 - STA 220+00
STA 233+13 - STA 307+78
STA 310+78 - STA 320+28
STA 338+28 - STA 369+28
STA 387+03 - STA 430+63



EXISTING TYPICAL SECTION
STH 38

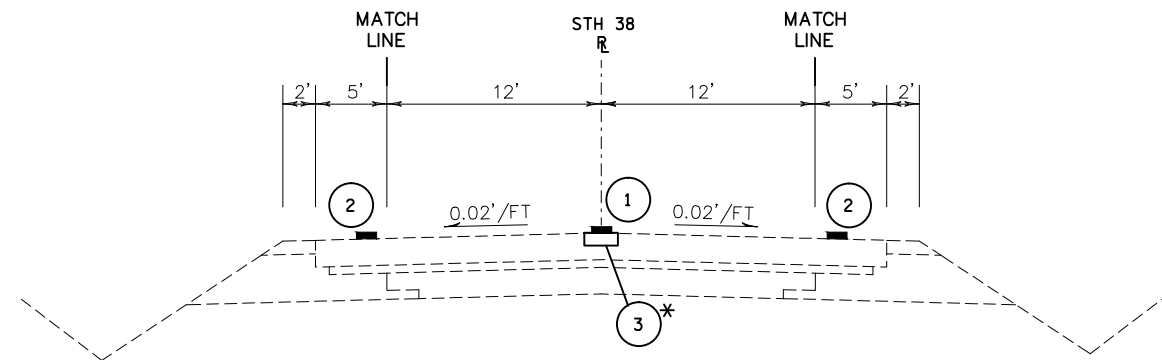
STA 108+00 - STA 124+00
STA 220+00 - STA 233+13
STA 307+78 - STA 310+78
STA 320+28 - STA 338+28
STA 369+28 - STA 387+03



EXISTING TYPICAL BYPASS LANE
STH 38

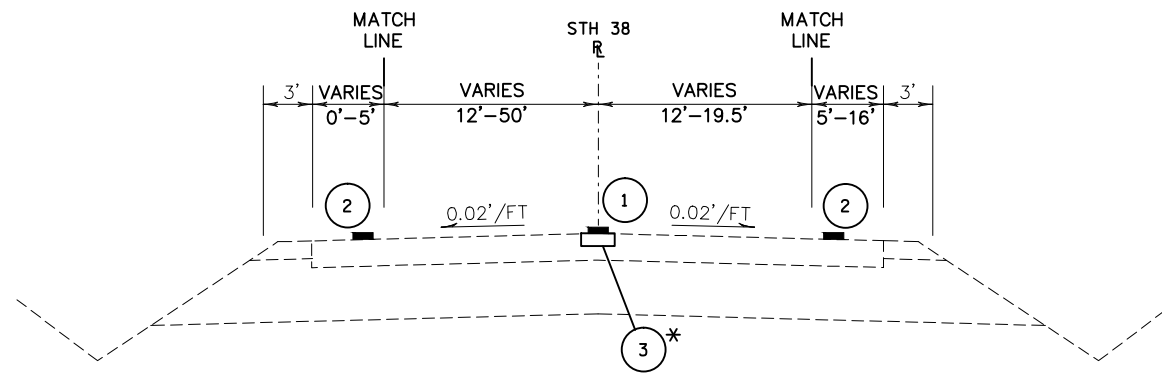
RT SHOWN, LT IS MIRROR IMAGE

STA. 43+52.5 - STA. 48+28 RT
STA. 45+60 - STA. 49+20 LT
STA. 63+37.5 - STA. 68+75 RT
STA. 63+85 - STA. 70+55 LT
STA. 169+62.5 - STA. 174+70 RT
STA. 172+10 - STA. 177+20 LT
STA. 213+05 - STA. 220+00 RT
STA. 213+75 - STA. 220+00 LT
STA. 360+50 - STA. 363+98 RT
STA. 416+65 - STA. 421+46 RT
STA. 417+58 - STA. 422+92 LT



FINISHED TYPICAL SECTION
STH 38

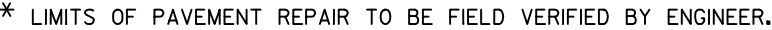
LONGITUDINAL JOINT REPAIR, RUMBLE STRIPS AND PAVEMENT MARKING
STA 35+40 - STA 108+00
STA 124+00 - STA 220+00
STA 233+13 - STA 307+78
STA 310+78 - STA 320+28
STA 338+28 - STA 369+28
STA 387+03 - STA 430+63



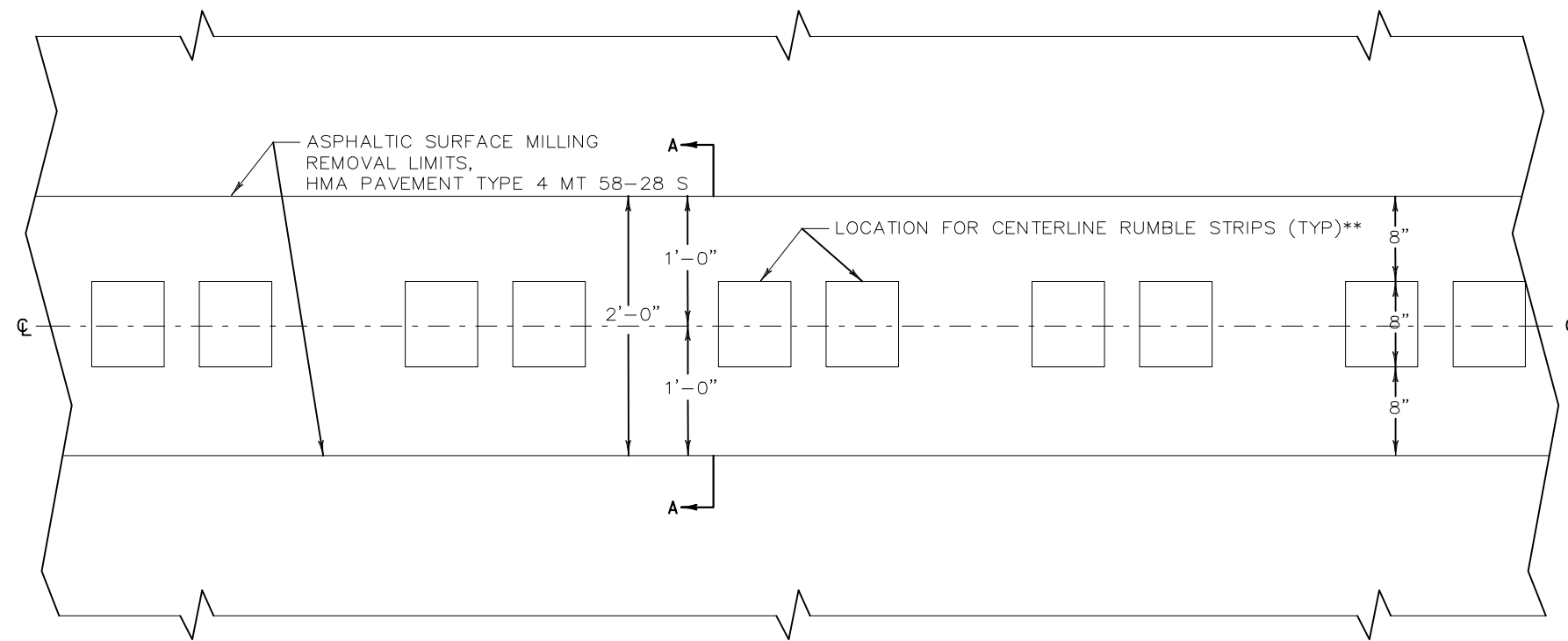
FINISHED TYPICAL SECTION
STH 38

LONGITUDINAL JOINT REPAIR, RUMBLE STRIPS AND PAVEMENT MARKING
STA 108+00 - STA 124+00
STA 220+00 - STA 233+13
STA 307+78 - STA 310+78
STA 320+28 - STA 338+28
STA 369+28 - STA 387+03

- 1 ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL
 - 2 ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL (TYPE 1)
 - 3 LONGITUDINAL JOINT REPAIR (SEE CONSTRUCTION DETAIL)
- * NO LONGITUDINAL JOINT REPAIR STA 262+35 - STA 385+45
EXACT STATIONING TO BE FIELD-VERIFIED BY ENGINEER.
SEE PLAN DETAILS FOR ADDITIONAL GAPS FOR LONGITUDINAL JOINT REPAIR.

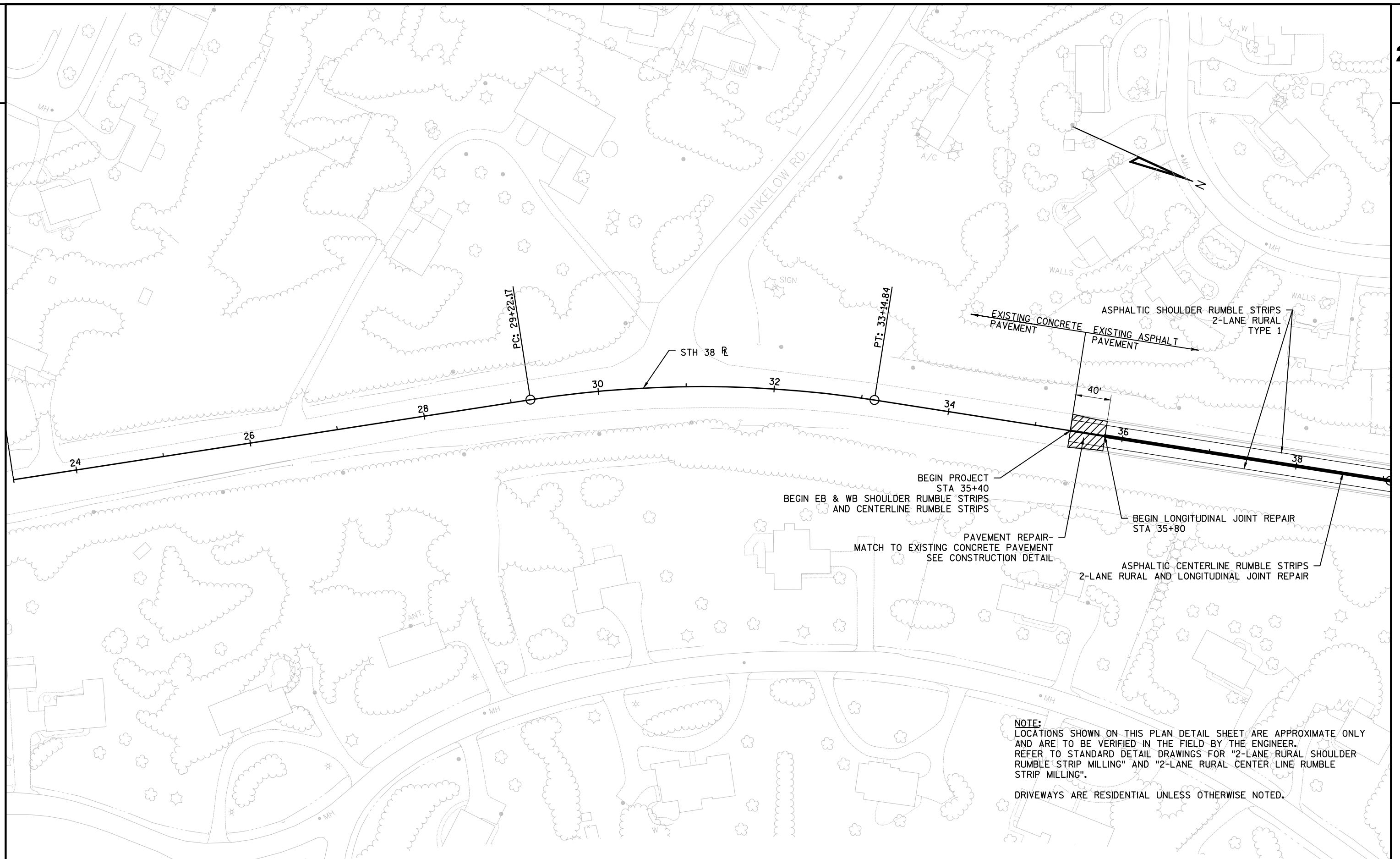


*** PLACE TEMPORARY PAVEMENT MARKING EPOXY 4-INCH
ON THE SAME DAY THAT THE NEW HMA PAVEMENT IS PLACED.



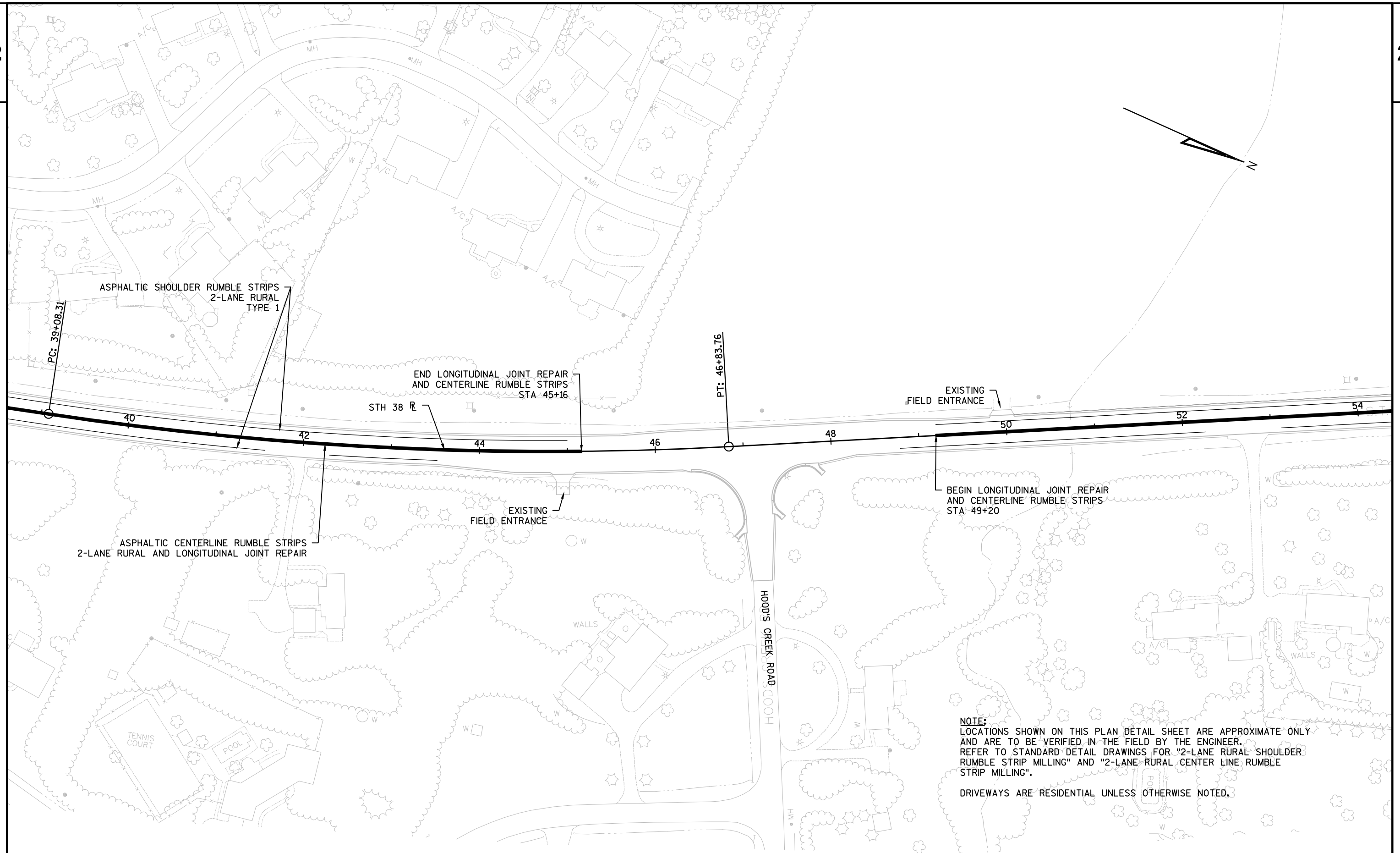
SECTION A-A
SUPERELEVATED ROADWAY

3



2

2



PROJECT NO:2060-17-70

HWY:STH 38

COUNTY: RACINE

PLAN DETAILS

SHEET

E

FILE NAME : N:\PDS\C3D\CAD\20601700\021200_PD.DWG
LAYOUT NAME - (02)_PD

PLOT DATE : 1/28/2016 6:03 PM PLOT BY : FERK, ANDREW C

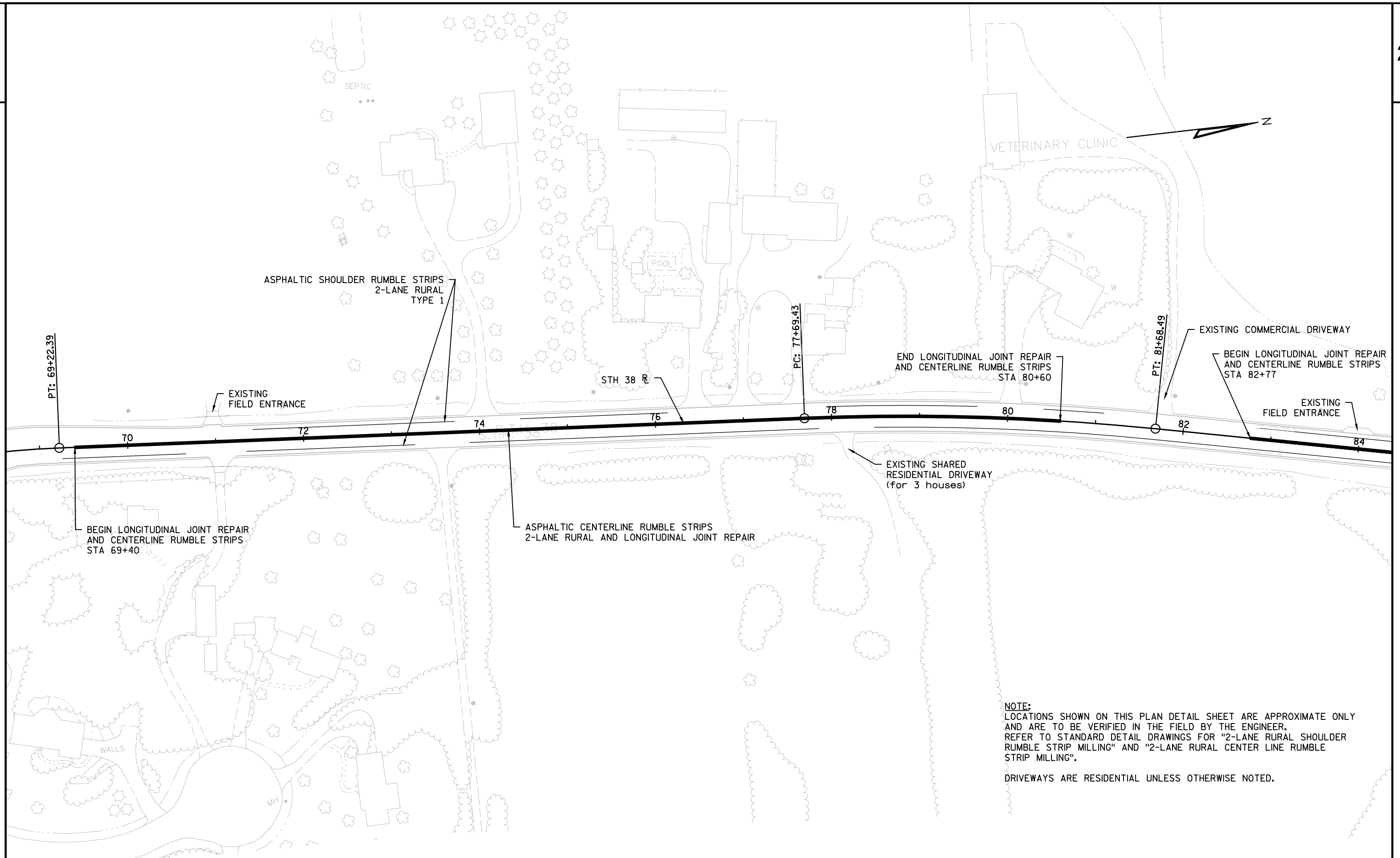
PLOT BY : FERK, ANDREW C

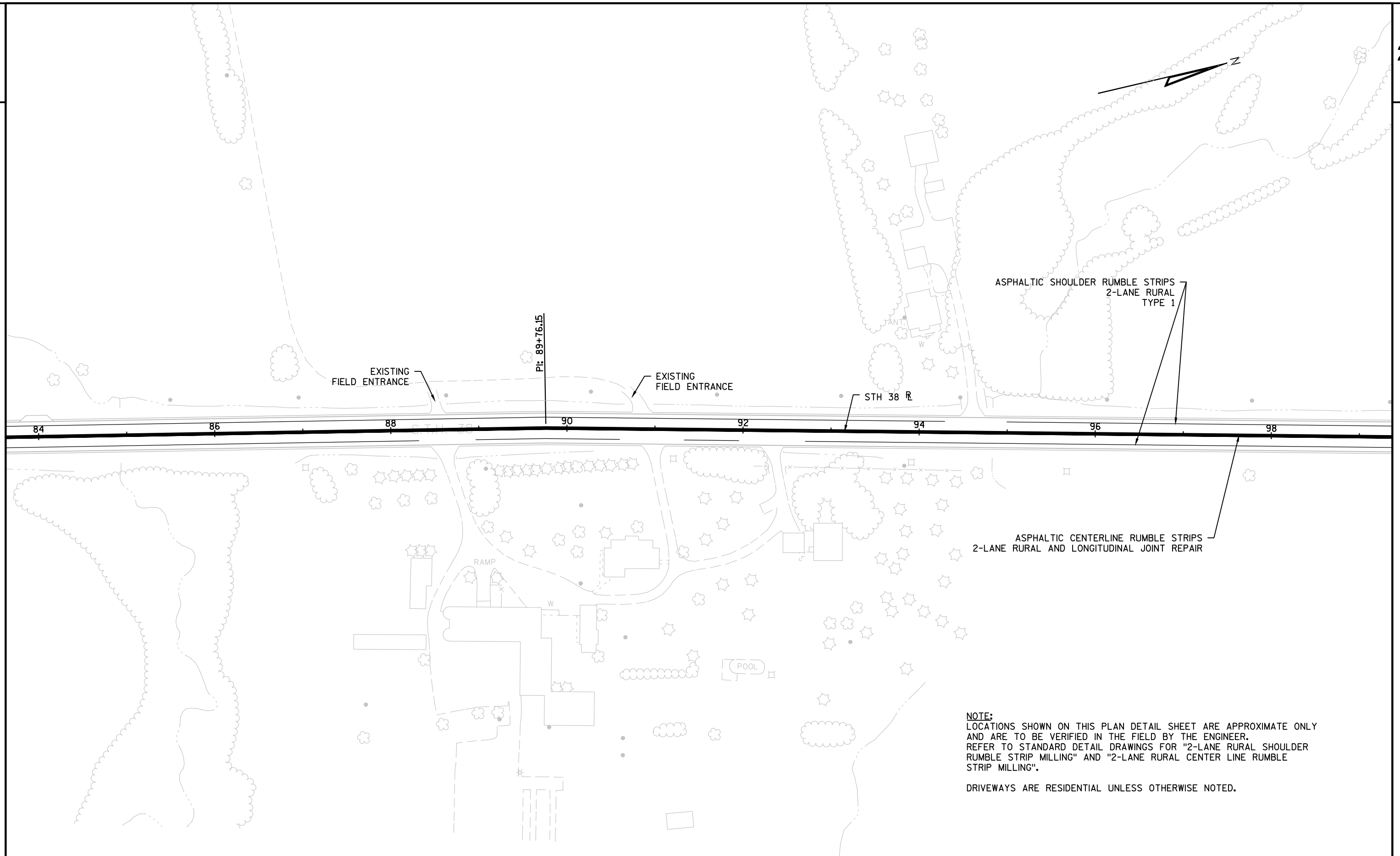
PLOT NAME :

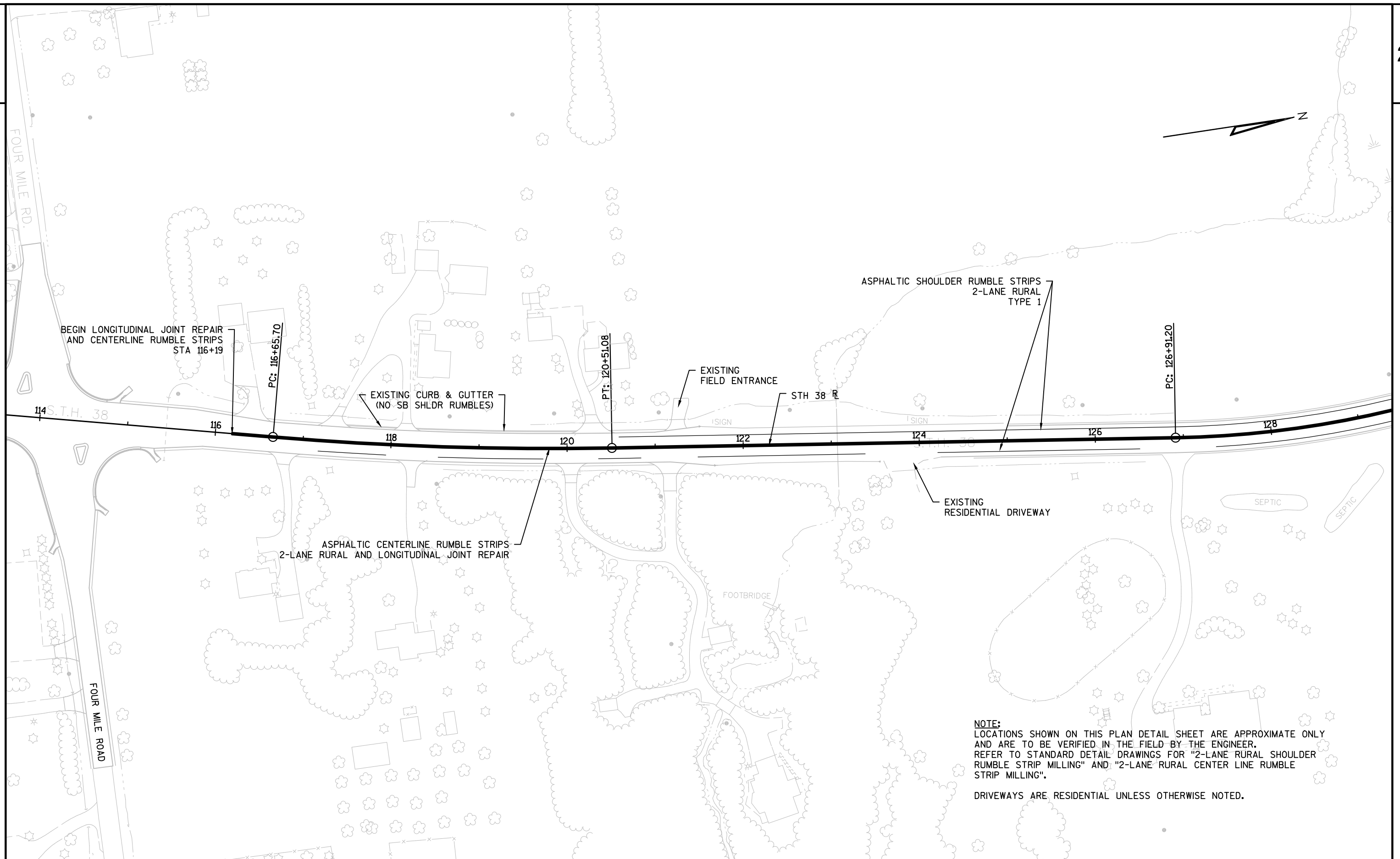
PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42









PROJECT NO: 2060-17-70

HWY: STH 38

COUNTY: RACINE

PLAN DETAILS

SHEET

E

FILE NAME : N:\PDS\C3D\CAD\20601700\021200_PD.DWG
LAYOUT NAME - (07)_PD

PLOT DATE : 1/28/2016 6:04 PM

PLOT BY : FERK, ANDREW C

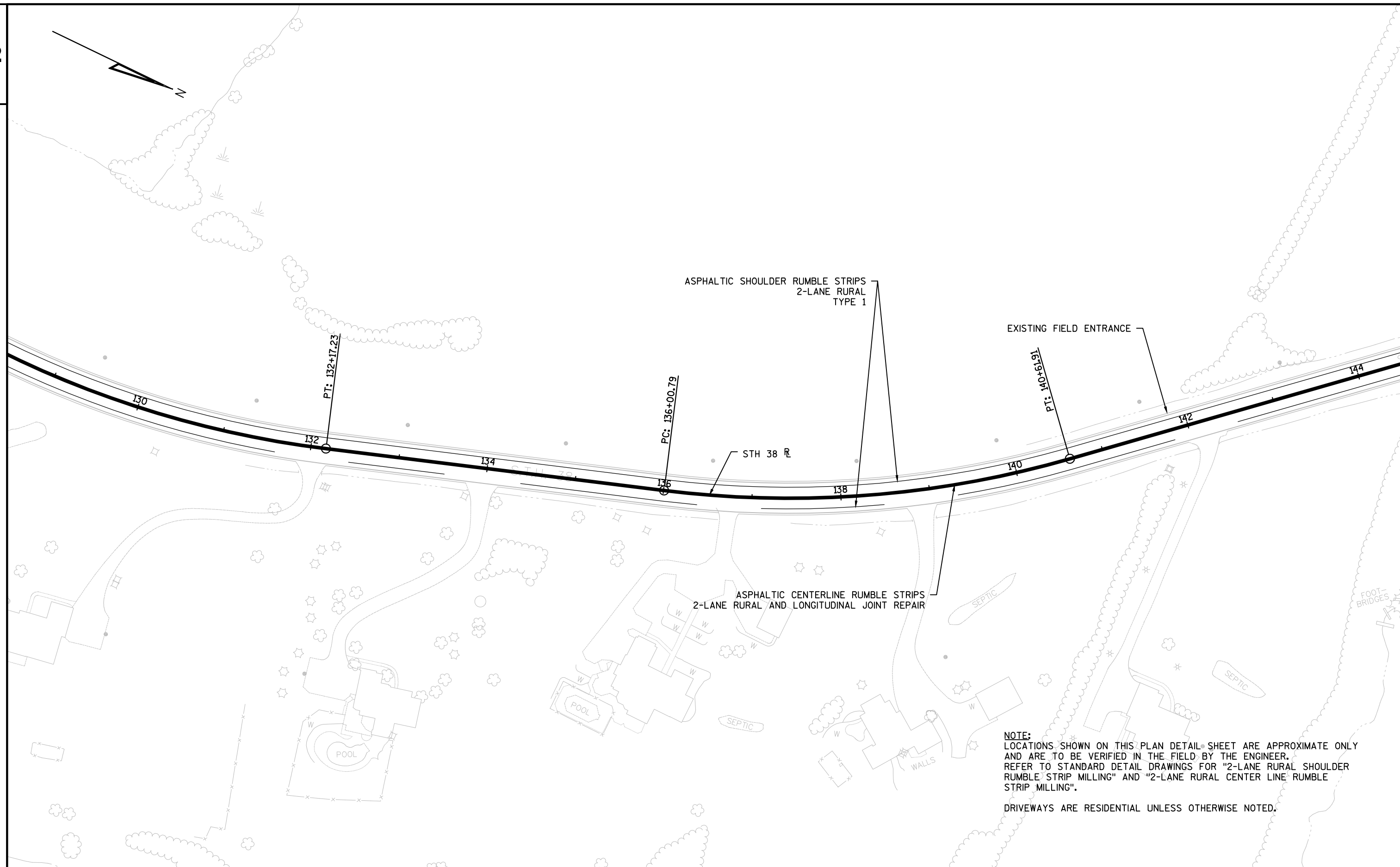
PLOT NAME :

PLOT SCALE : 1 IN:100 FT

WISDOT/CADDs SHEET 42

2

2



PROJECT NO: 2060-17-70

HWY:STH 38

COUNTY: RACINE

PLAN DETAILS	
--------------	--

SHEET

E

FILE NAME : N:\PDS\C3D\CAD\20601700\021200_PD.DWG
LAYOUT NAME - (08)_PD

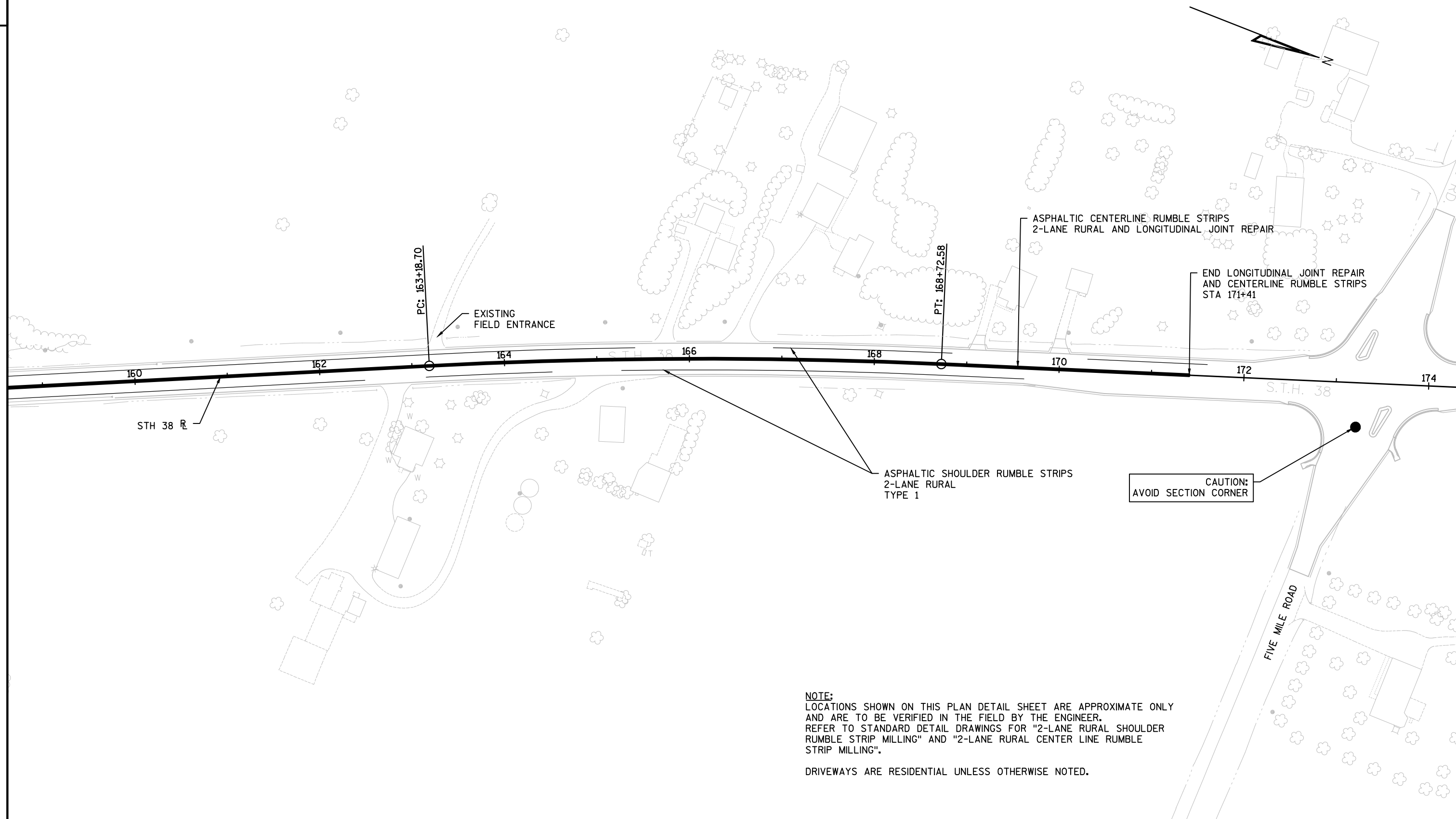
PLOT DATE : 1/28/2016 6:04 PM PLOT BY : FERK, ANDREW C

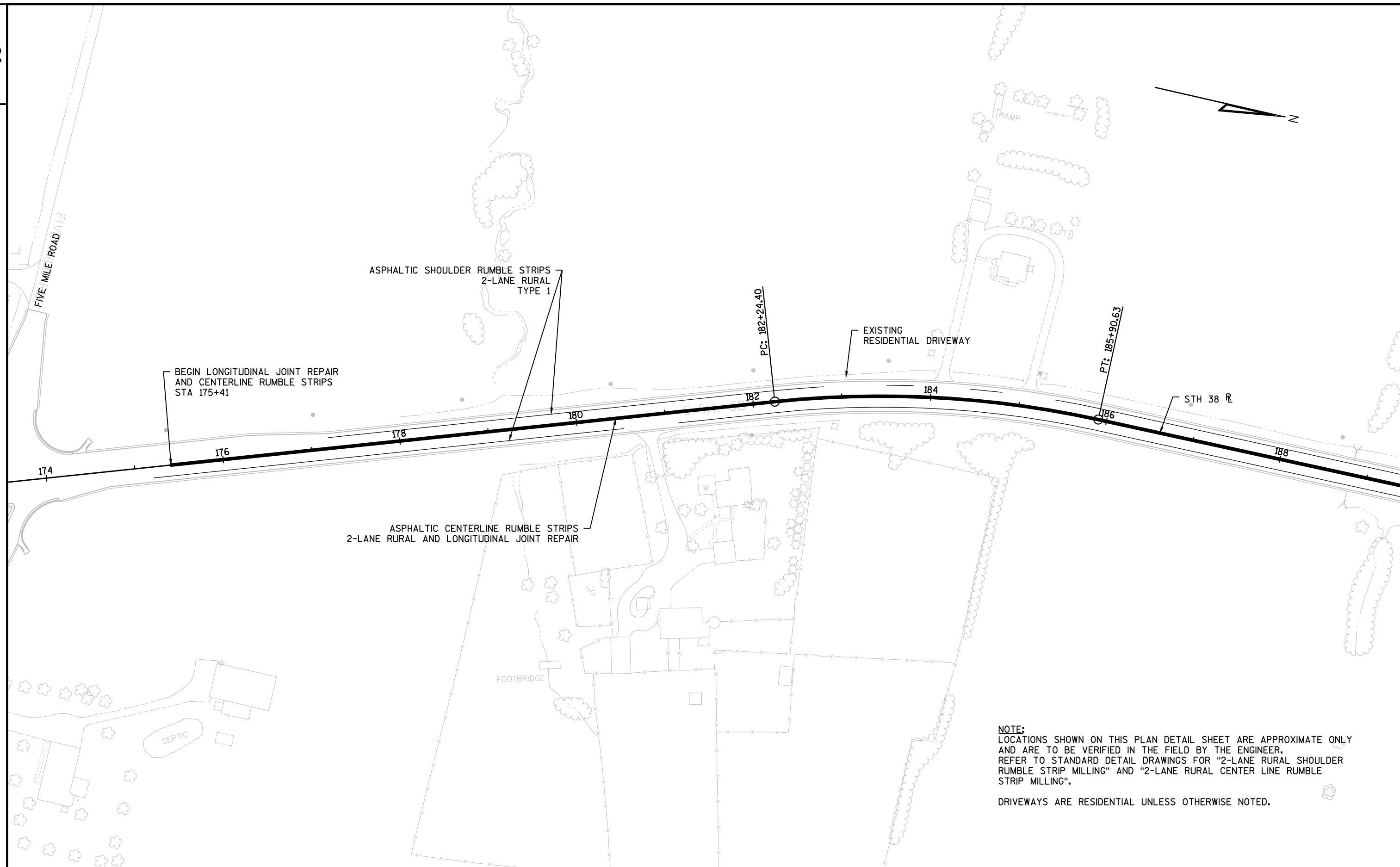
PLOT BY : FERK, ANDREW C

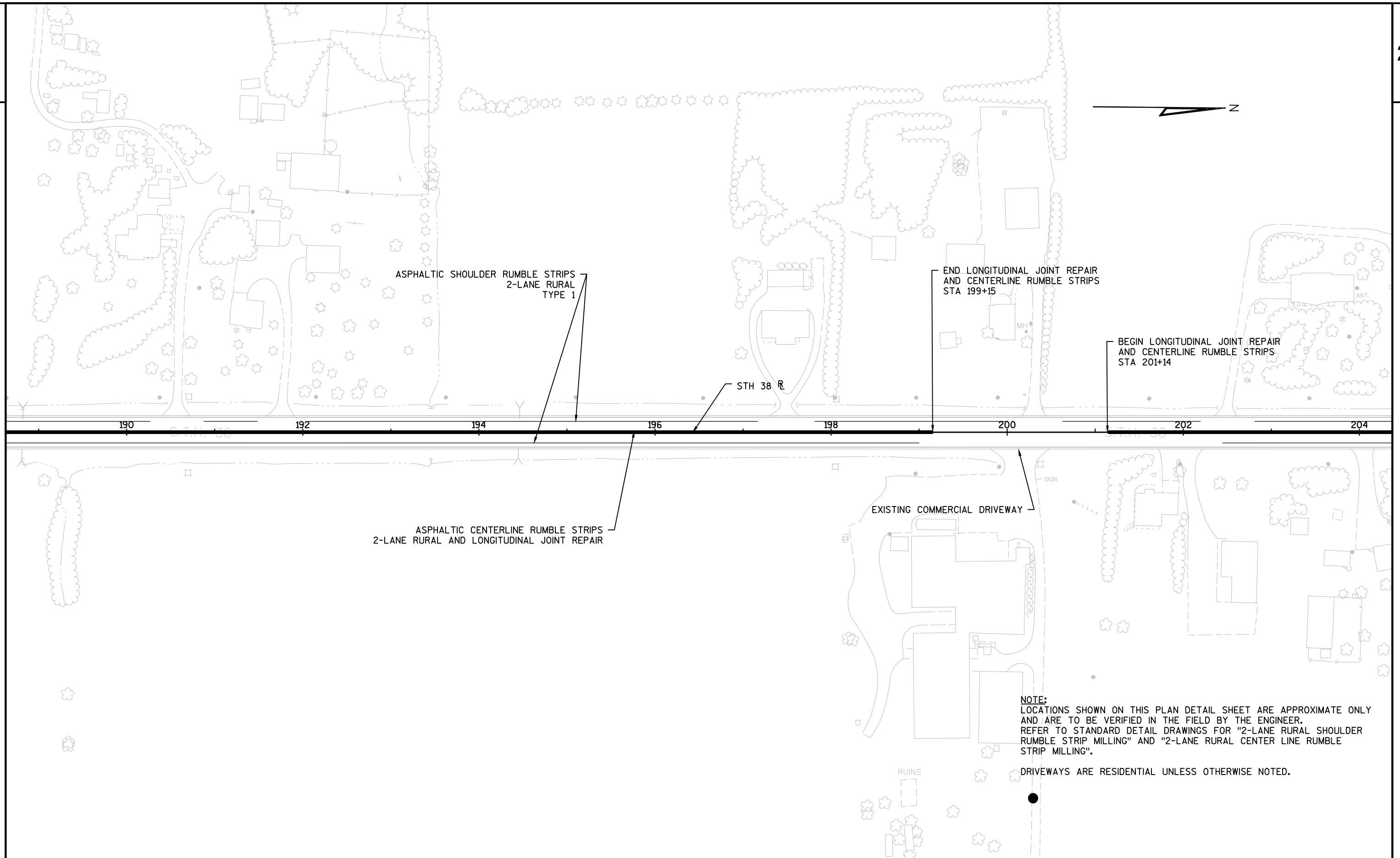
PLOT NAME : PLOT SCALE : 1 IN:100 FT

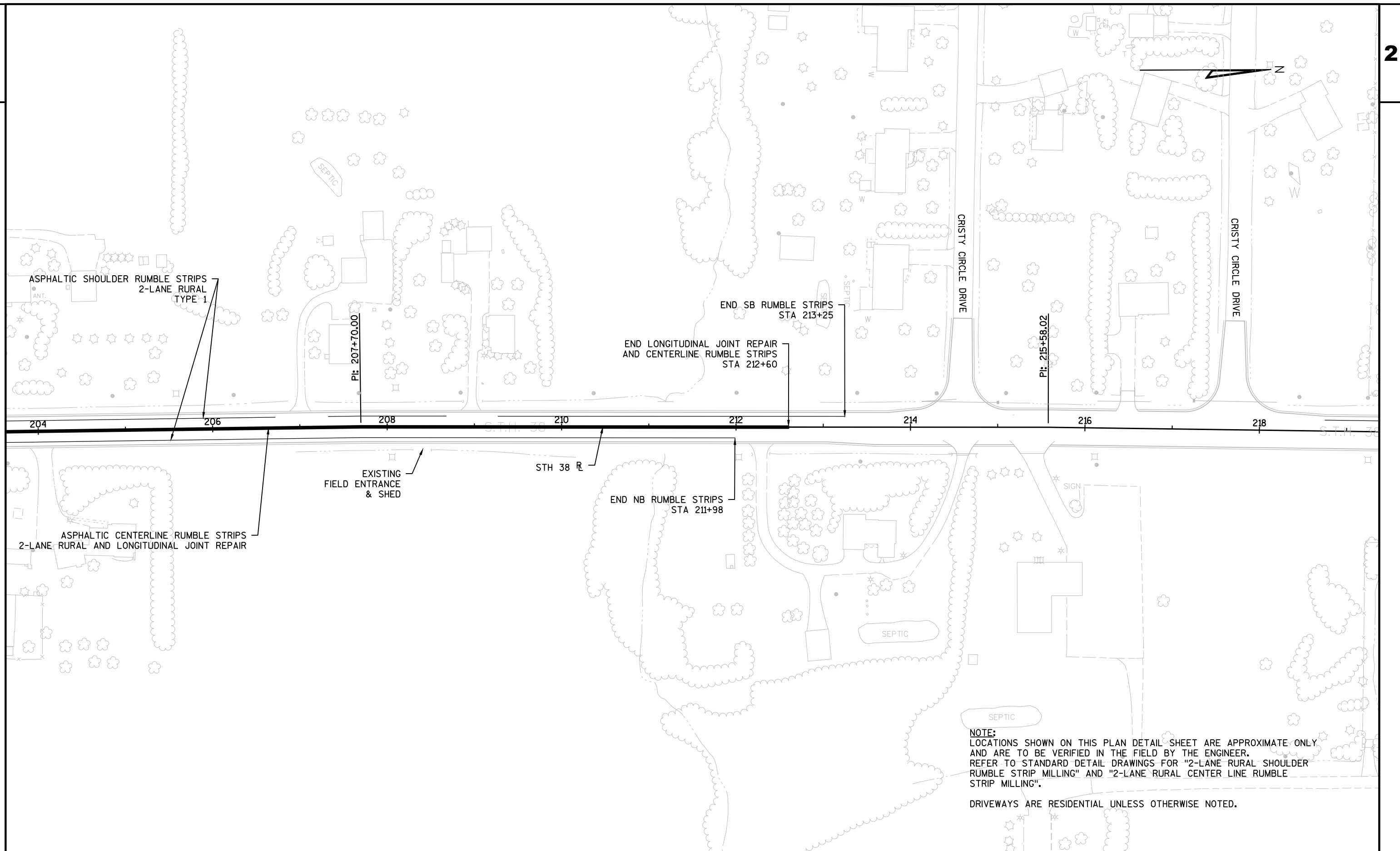
PLOT SCALE : 1 IN:100 FT

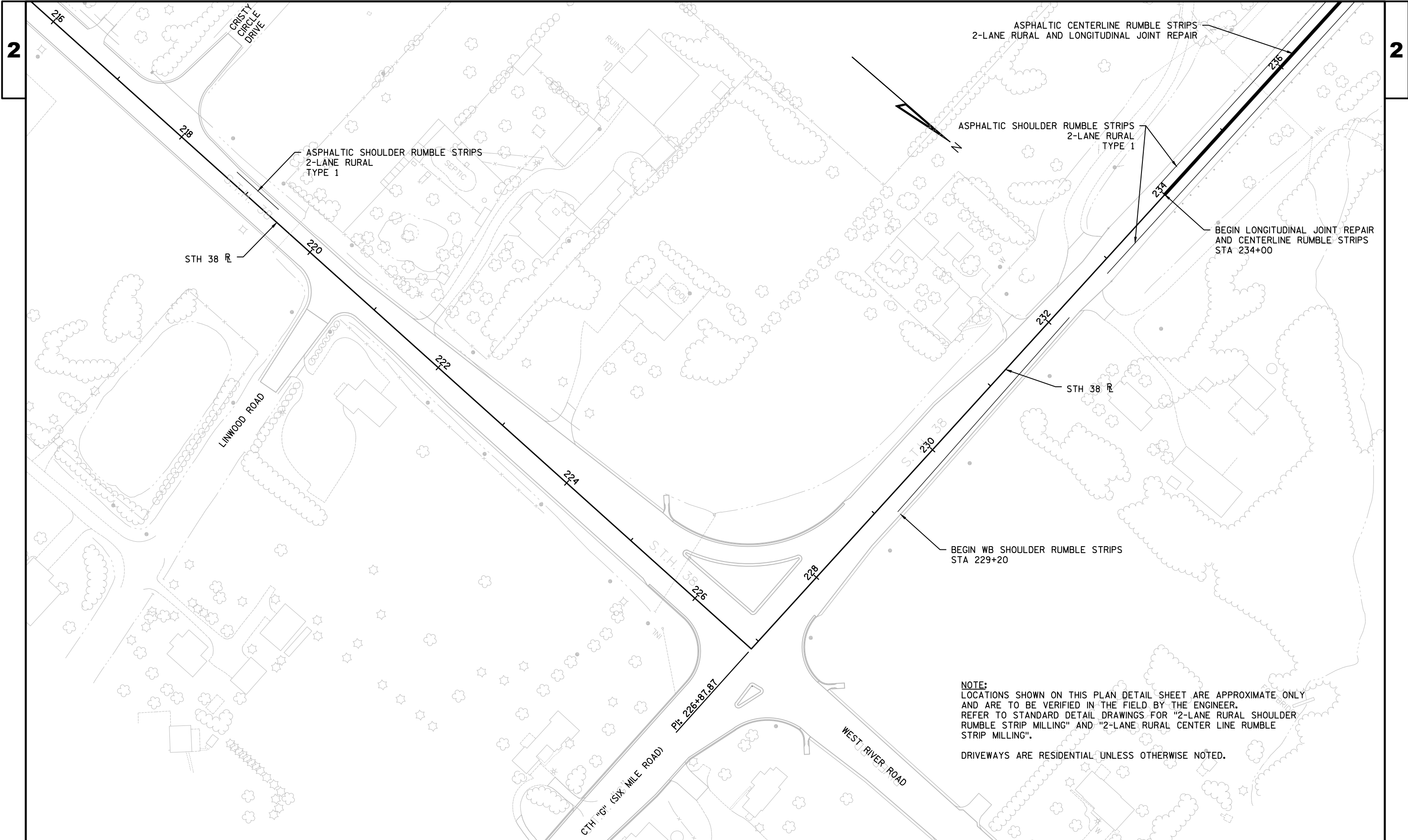
WISDOT/CADDS SHEET 42





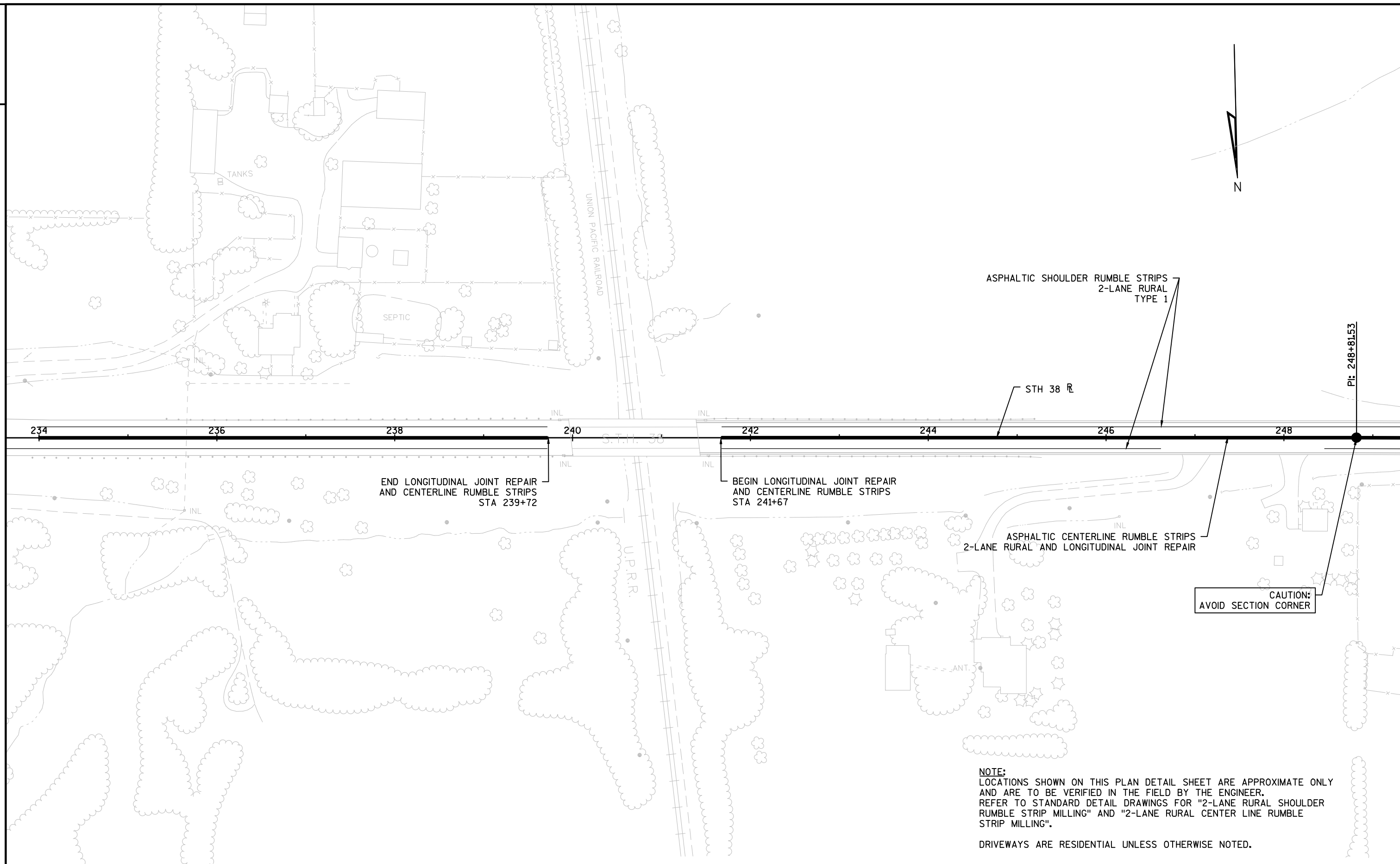


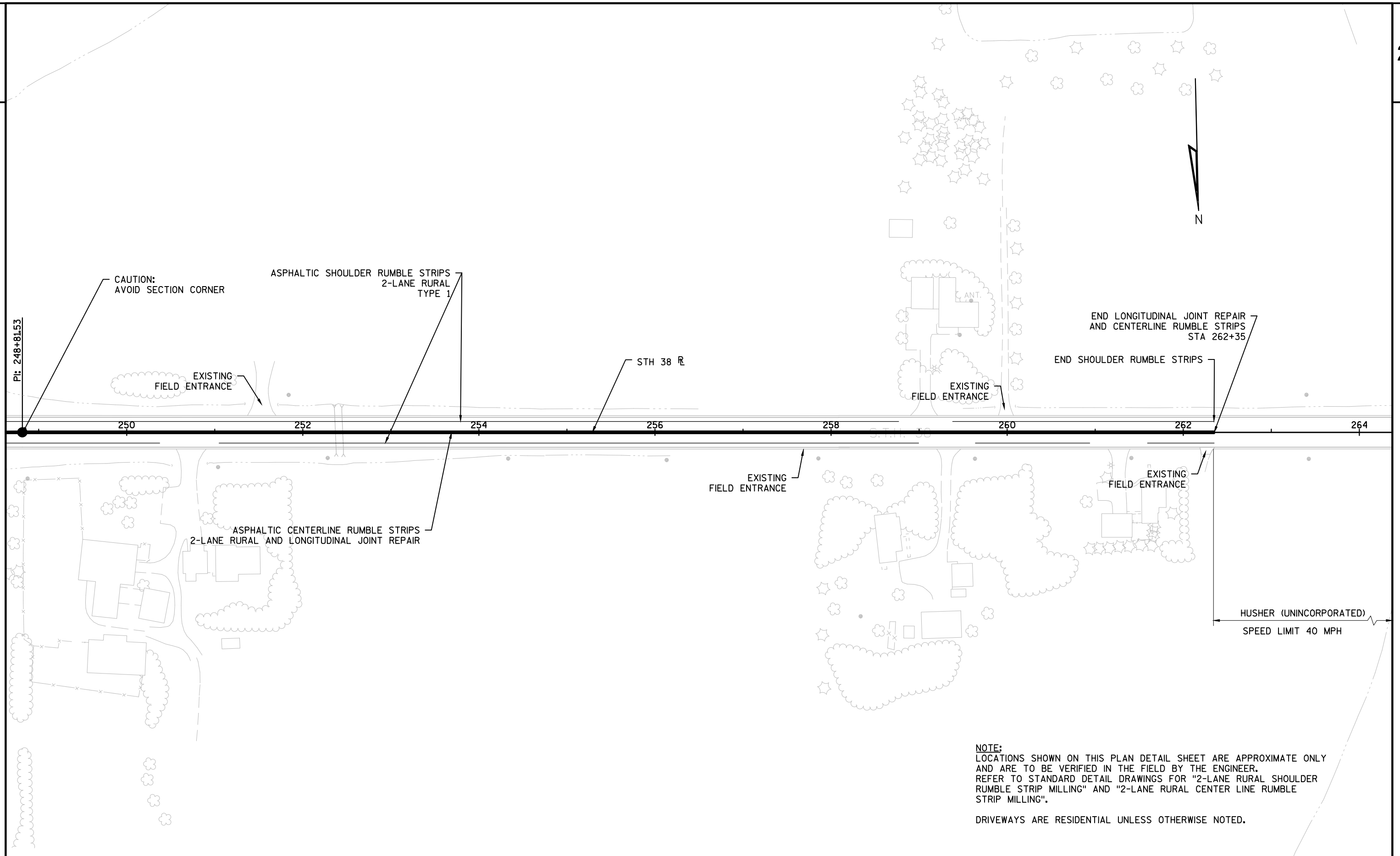




NOTE:
LOCATIONS SHOWN ON THIS PLAN DETAIL SHEET ARE APPROXIMATE ONLY
AND ARE TO BE VERIFIED IN THE FIELD BY THE ENGINEER.
REFER TO STANDARD DETAIL DRAWINGS FOR "2-LANE RURAL SHOULDER
RUMBLE STRIP MILLING" AND "2-LANE RURAL CENTER LINE RUMBLE
STRIP MILLING".

DRIVEWAYS ARE RESIDENTIAL UNLESS OTHERWISE NOTED.





PROJECT NO:2060-17-70

HWY:STH 38

COUNTY:RACINE

PLAN DETAILS

SHEET

E

FILE NAME : N:\PDS\C3D\CAD\20601700\021200_PD.DWG
LAYOUT NAME - (16)_PD

PLOT DATE : 1/28/2016 6:06 PM

PLOT BY : FERK, ANDREW C

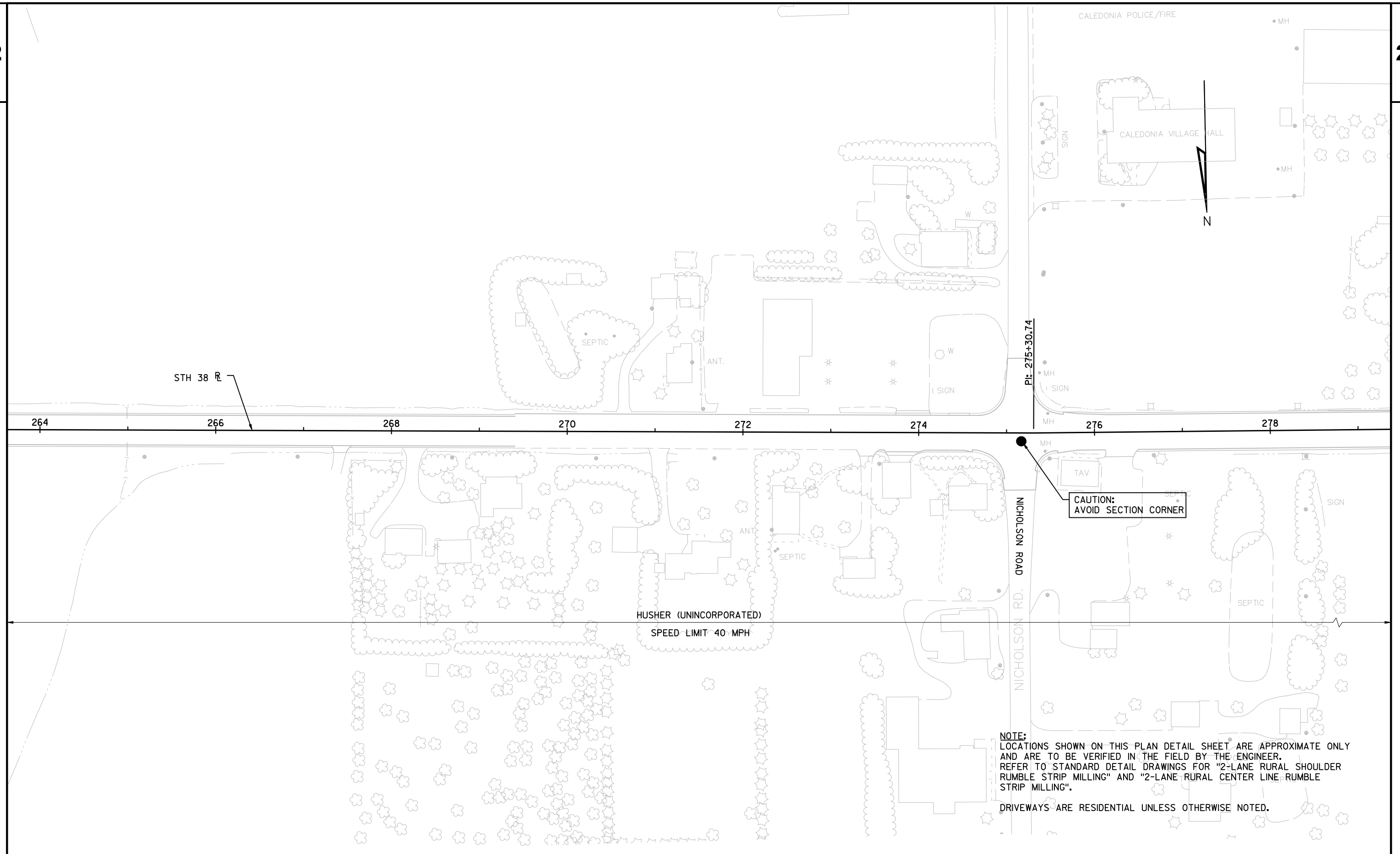
PLOT NAME :

PLOT SCALE : 1 IN:100 FT

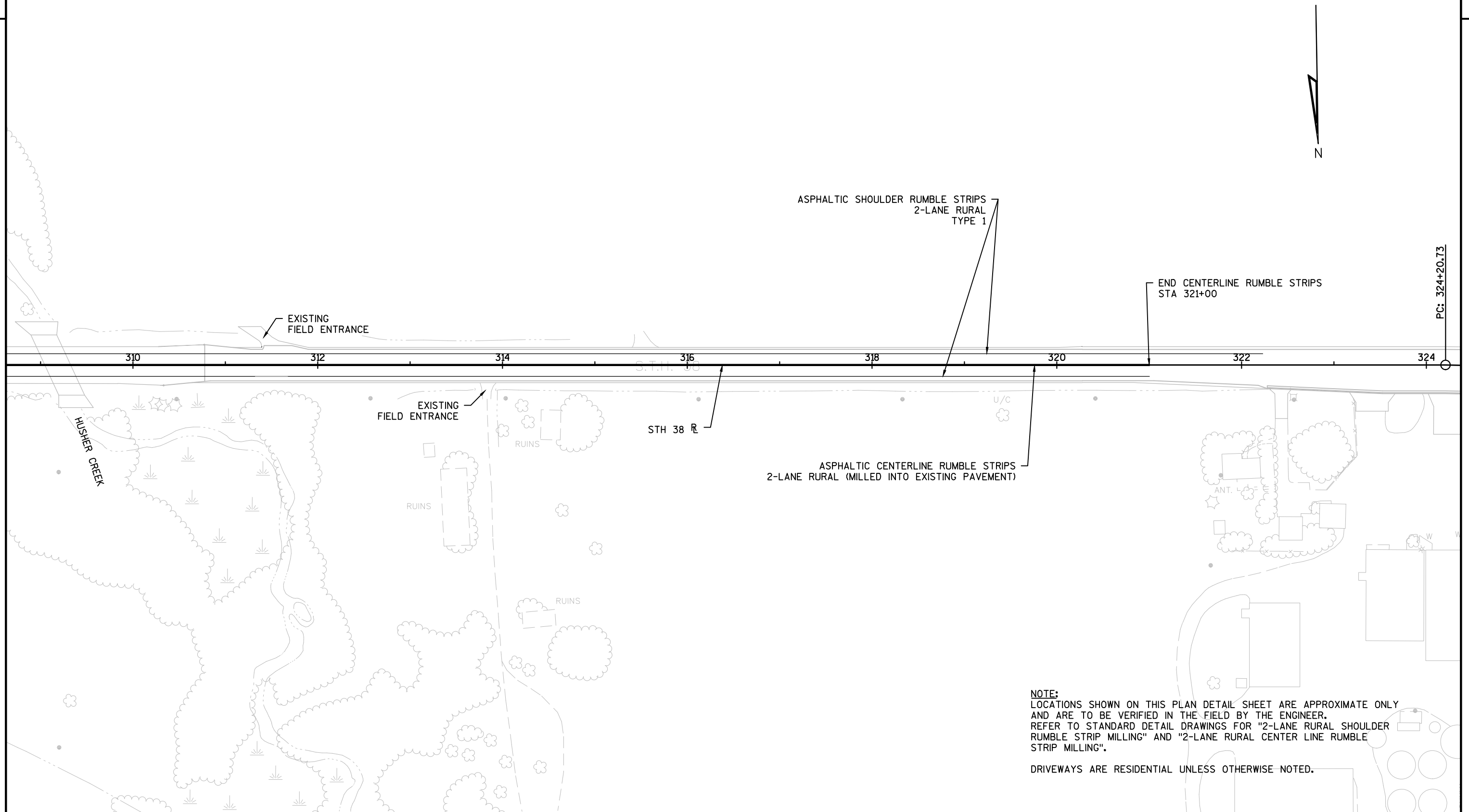
WISDOT/CADDs SHEET 42

2

2

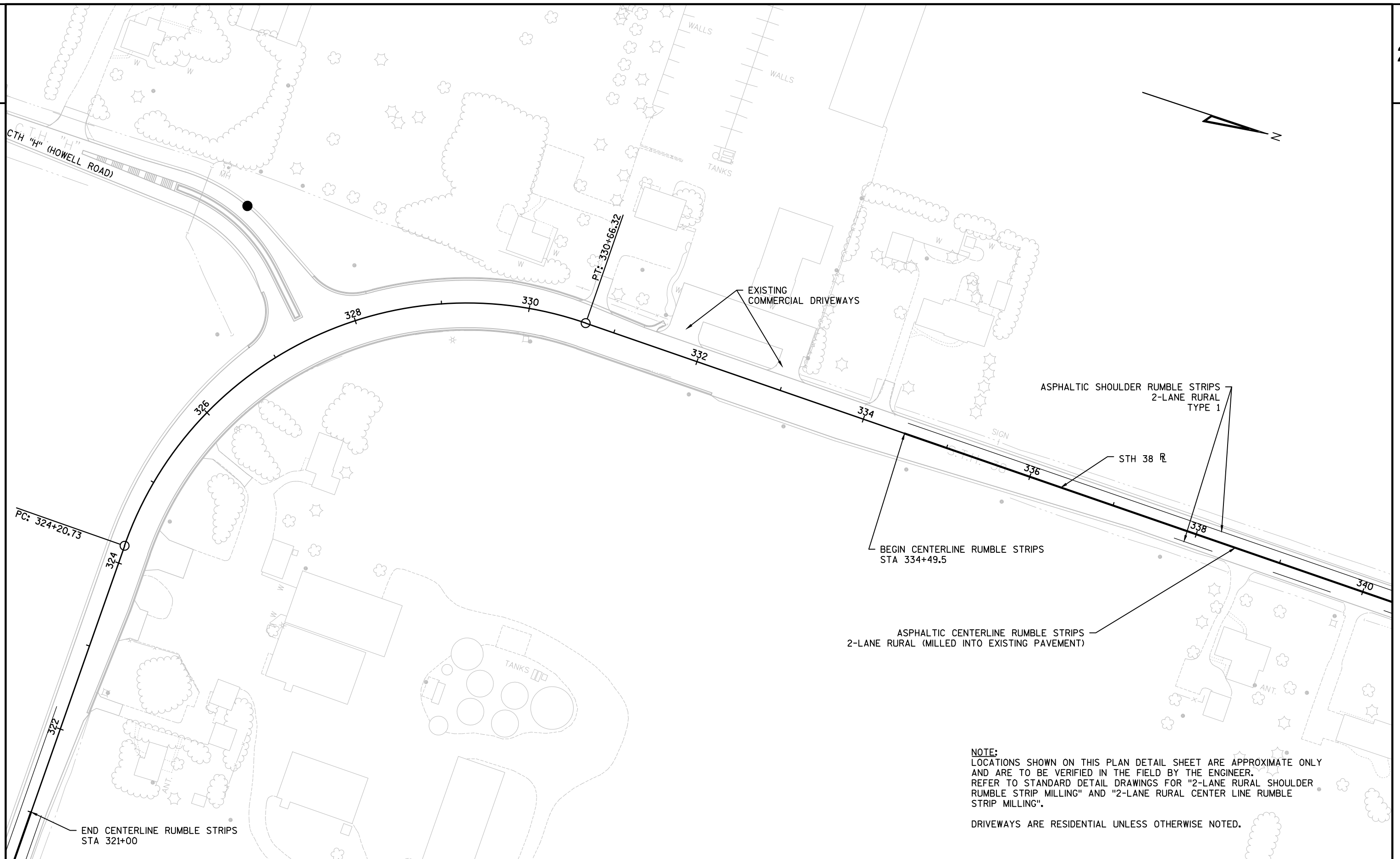


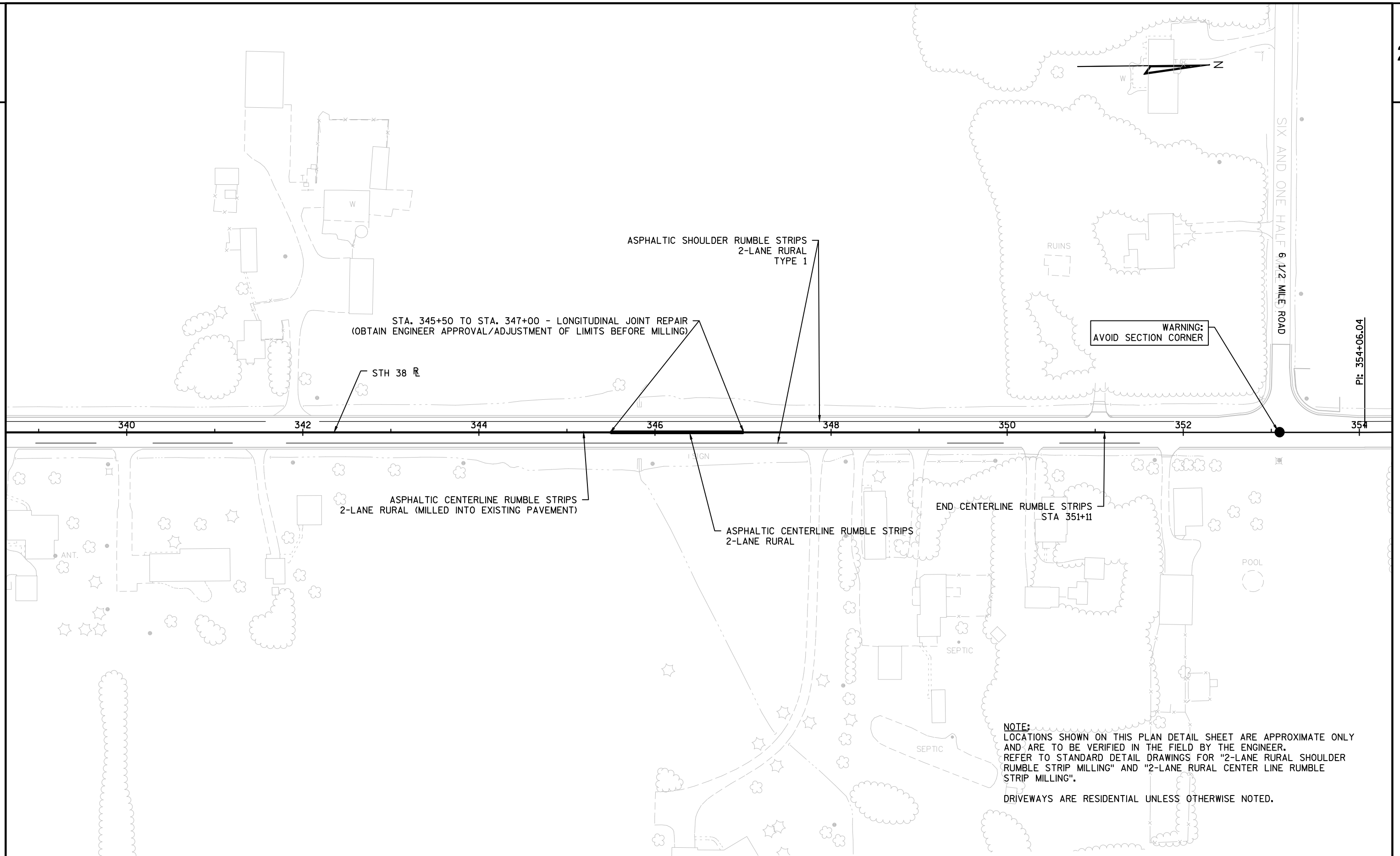
PROJECT NO:2060-17-70	HWY:STH 38	COUNTY:RACINE	PLAN DETAILS	SHEET	E
-----------------------	------------	---------------	--------------	-------	----------

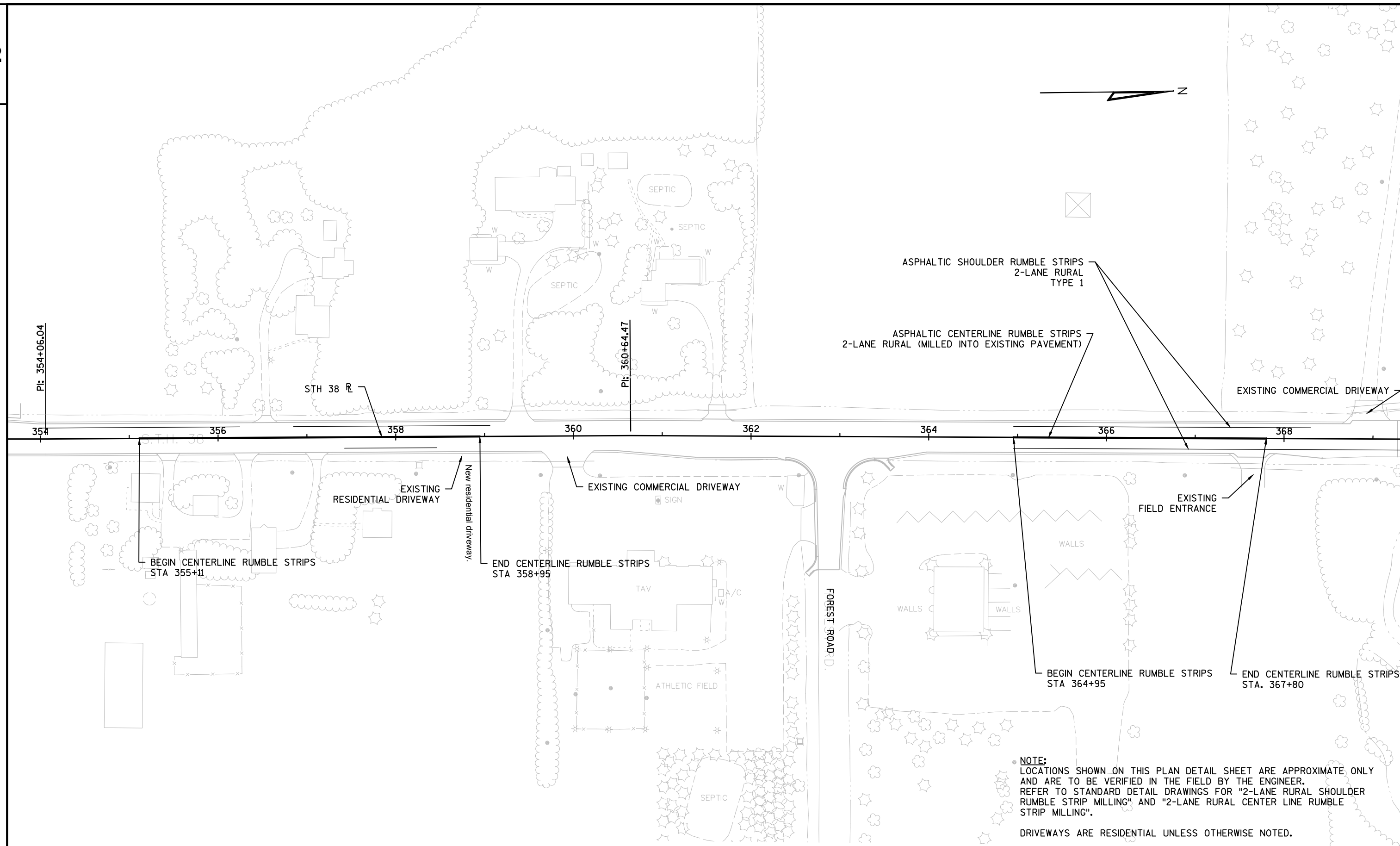


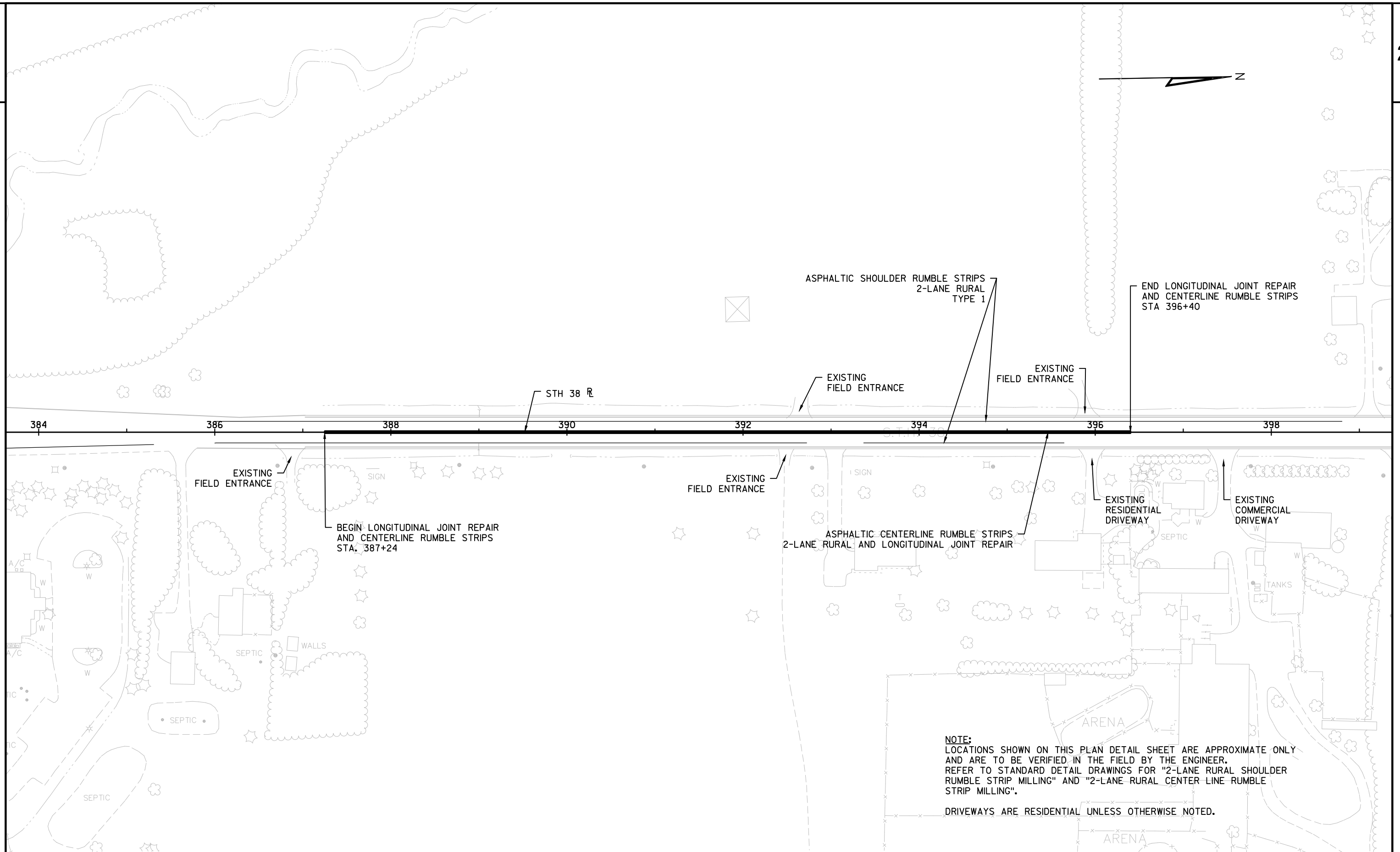
2

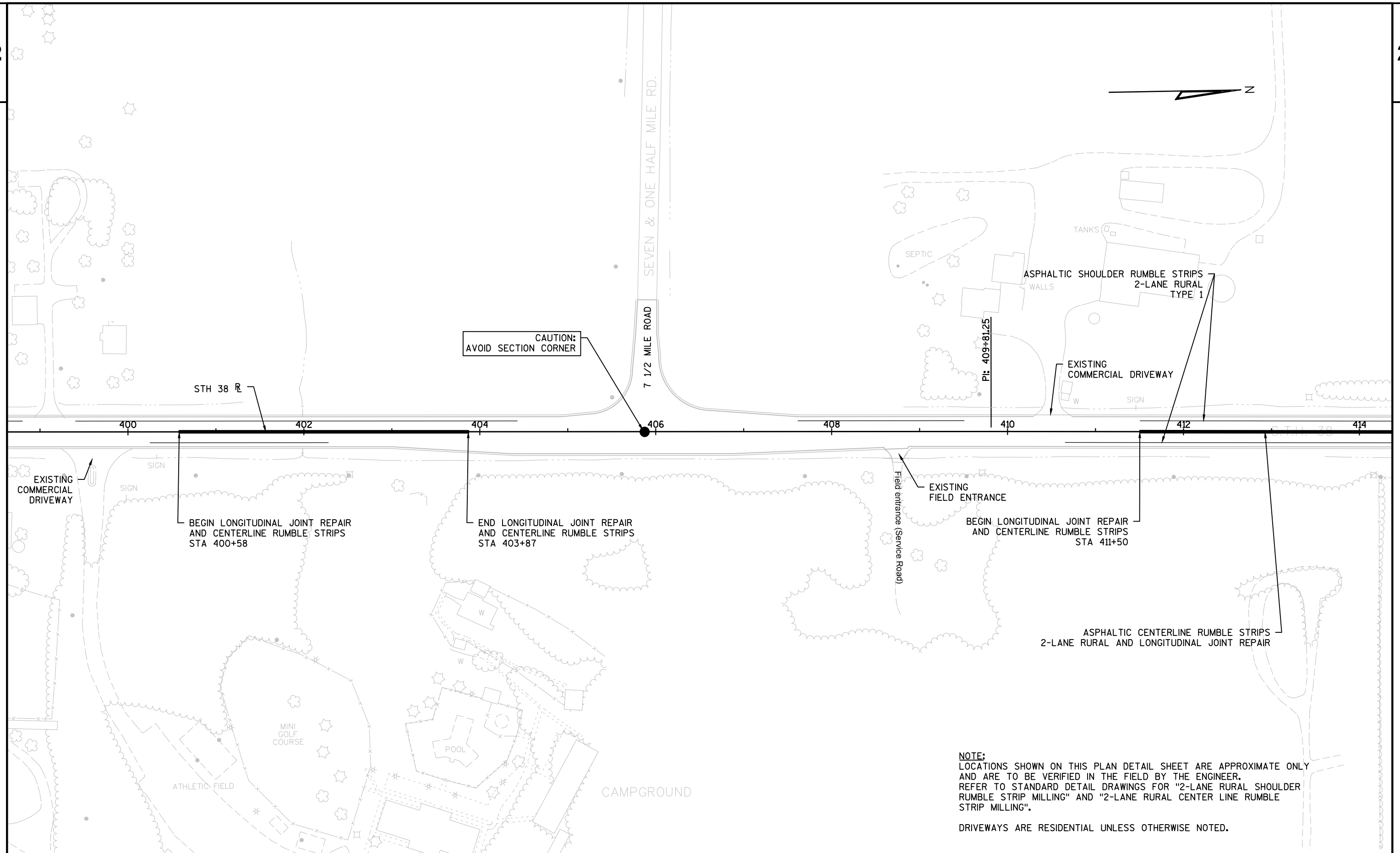
2

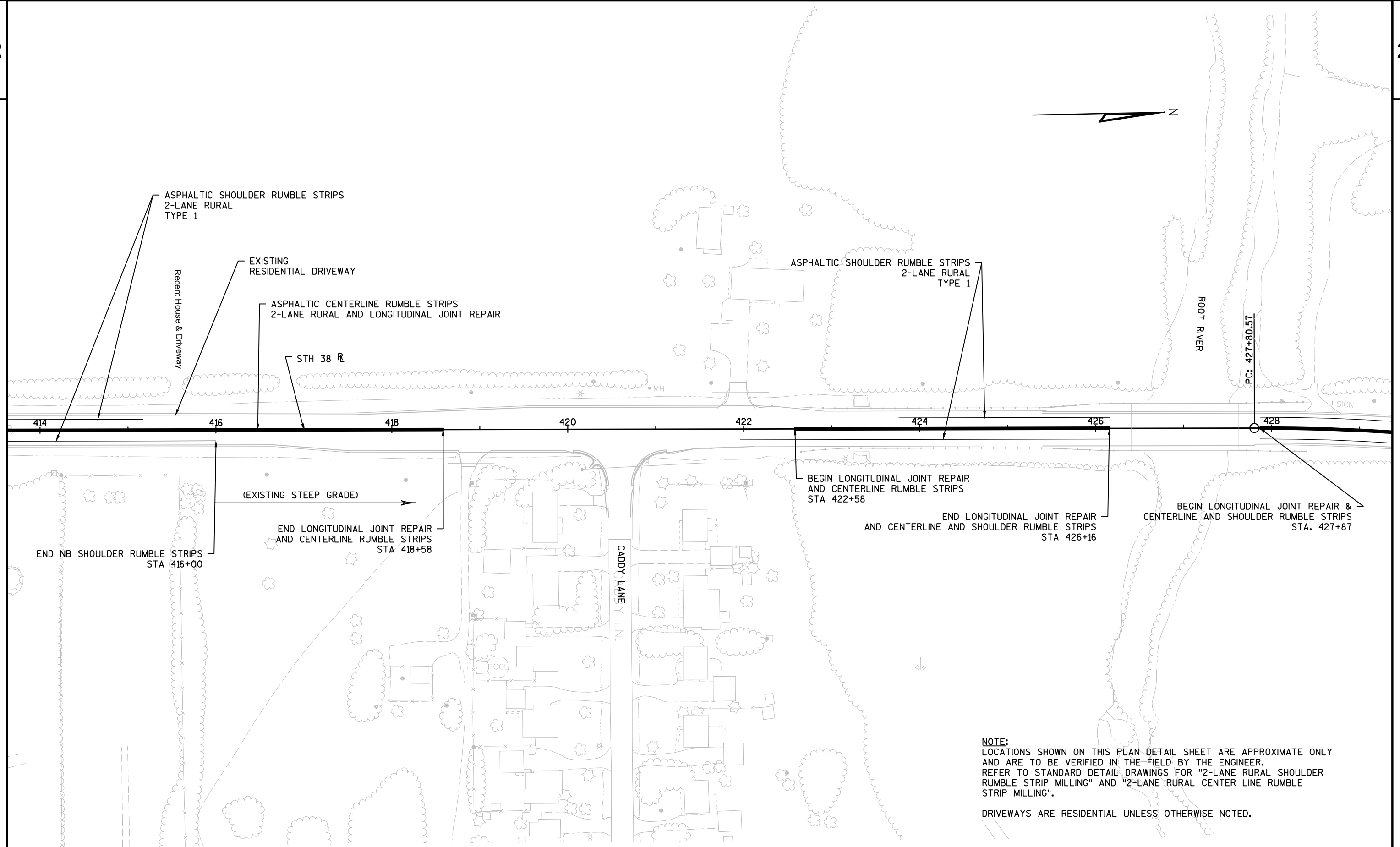


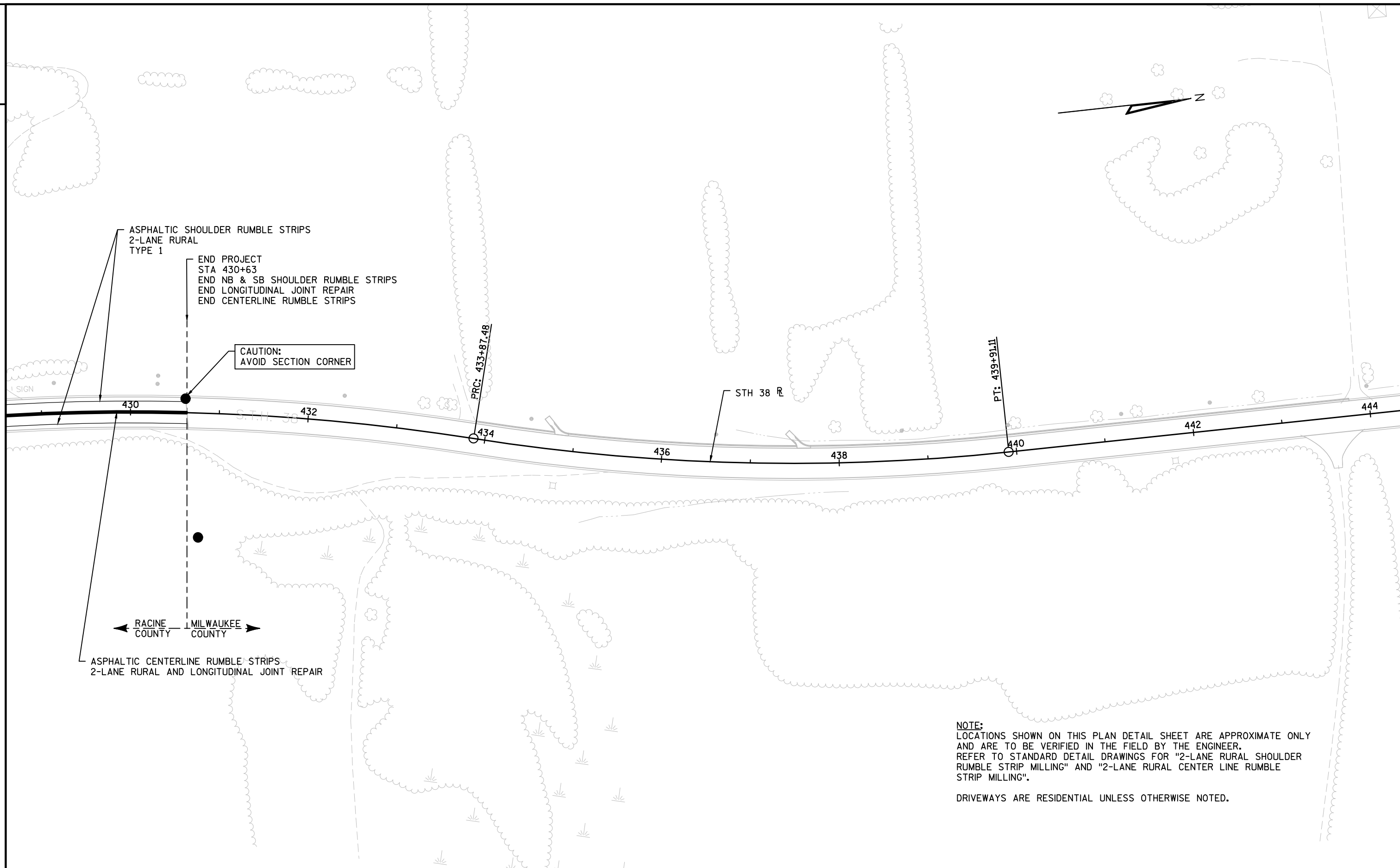


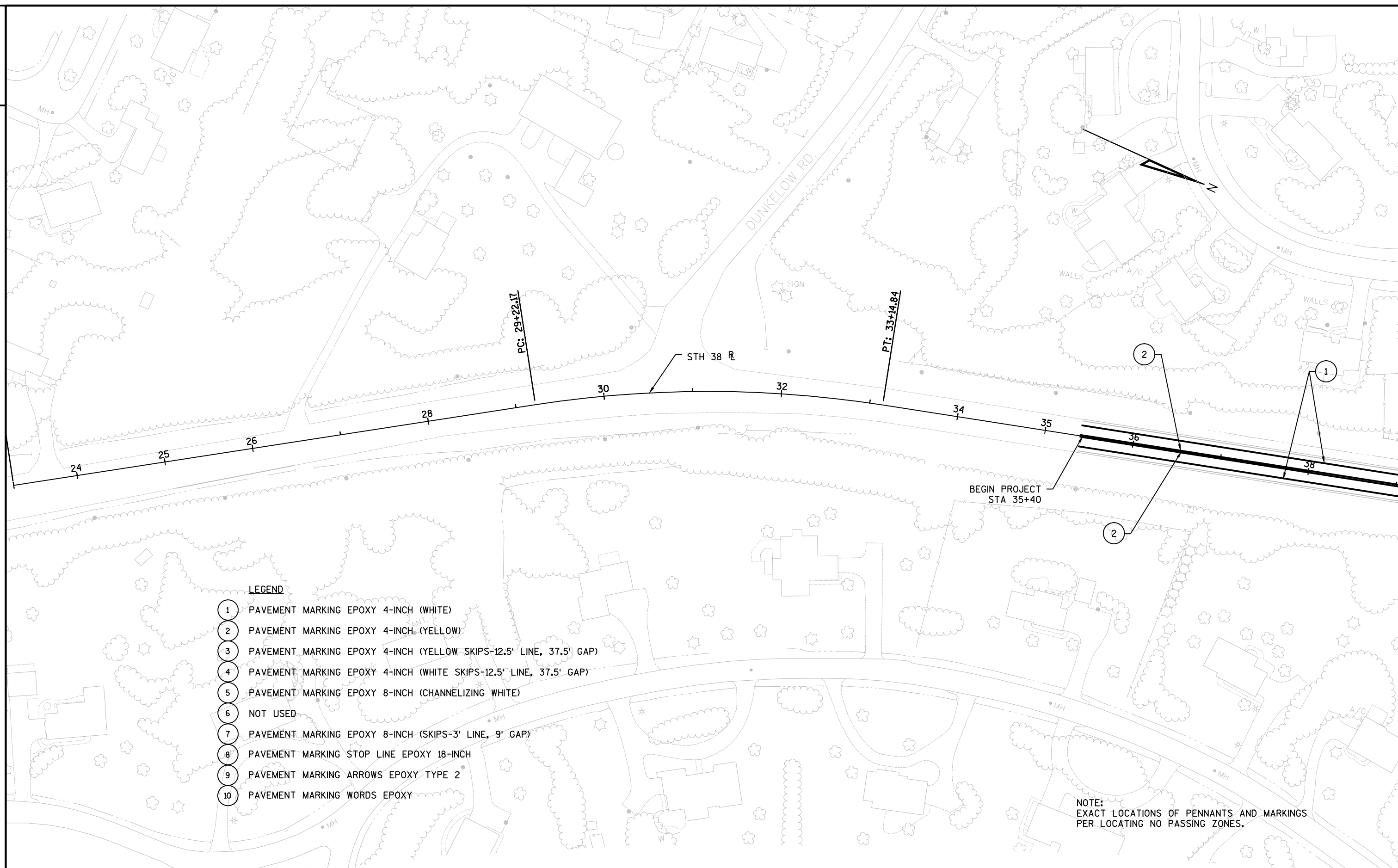


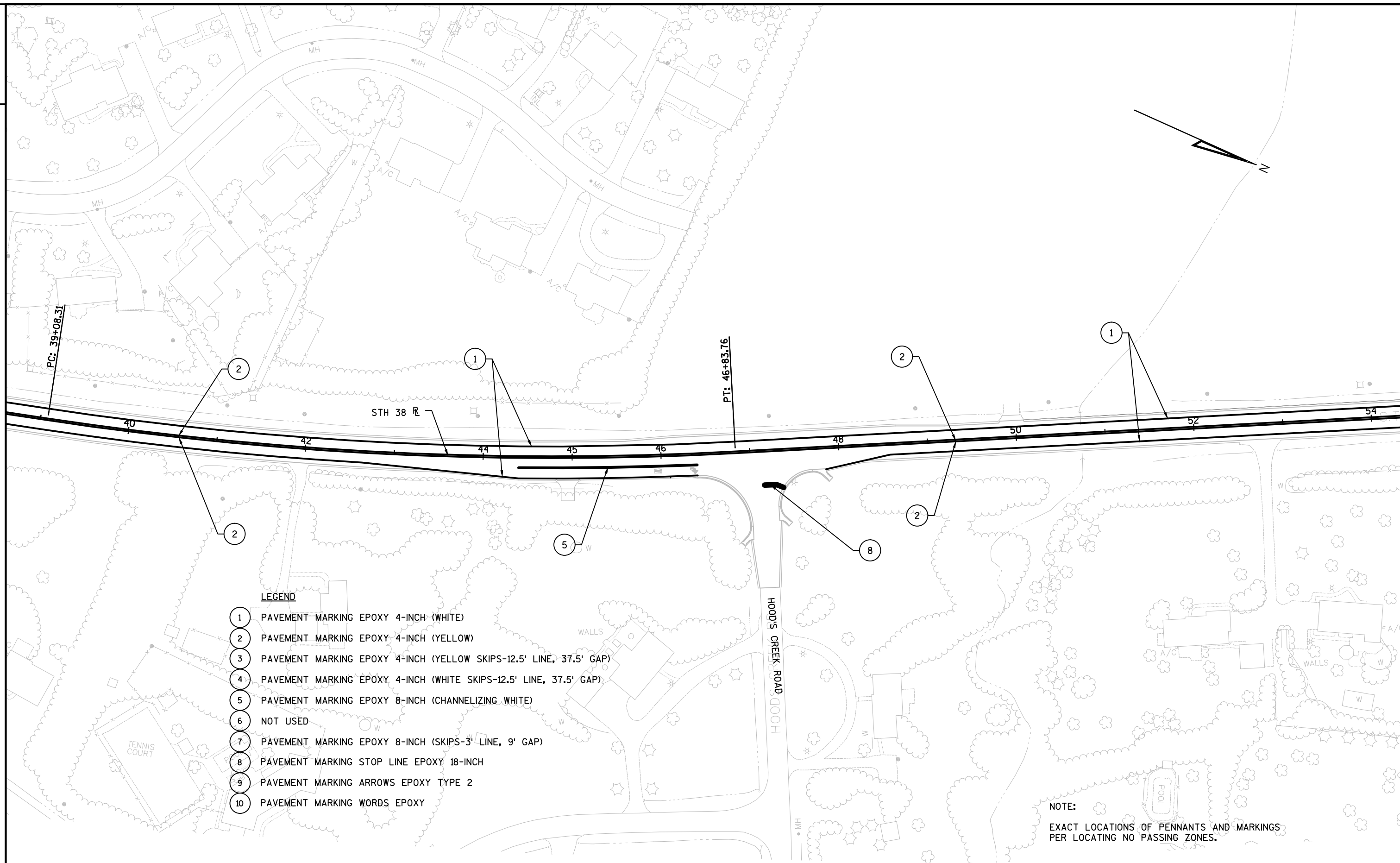


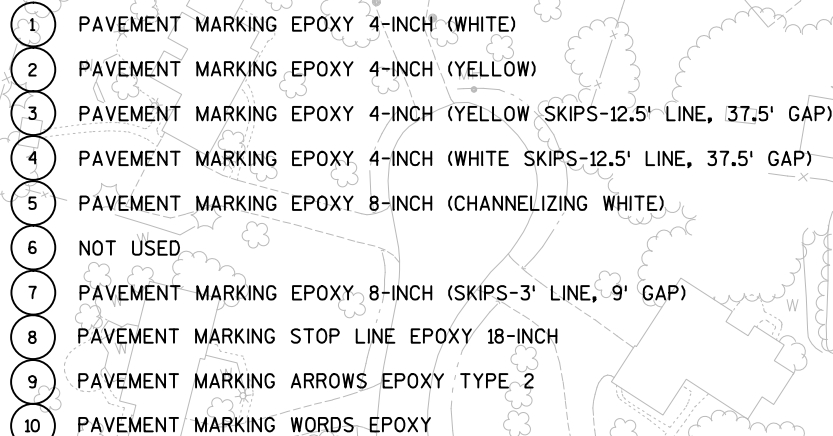






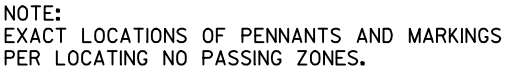


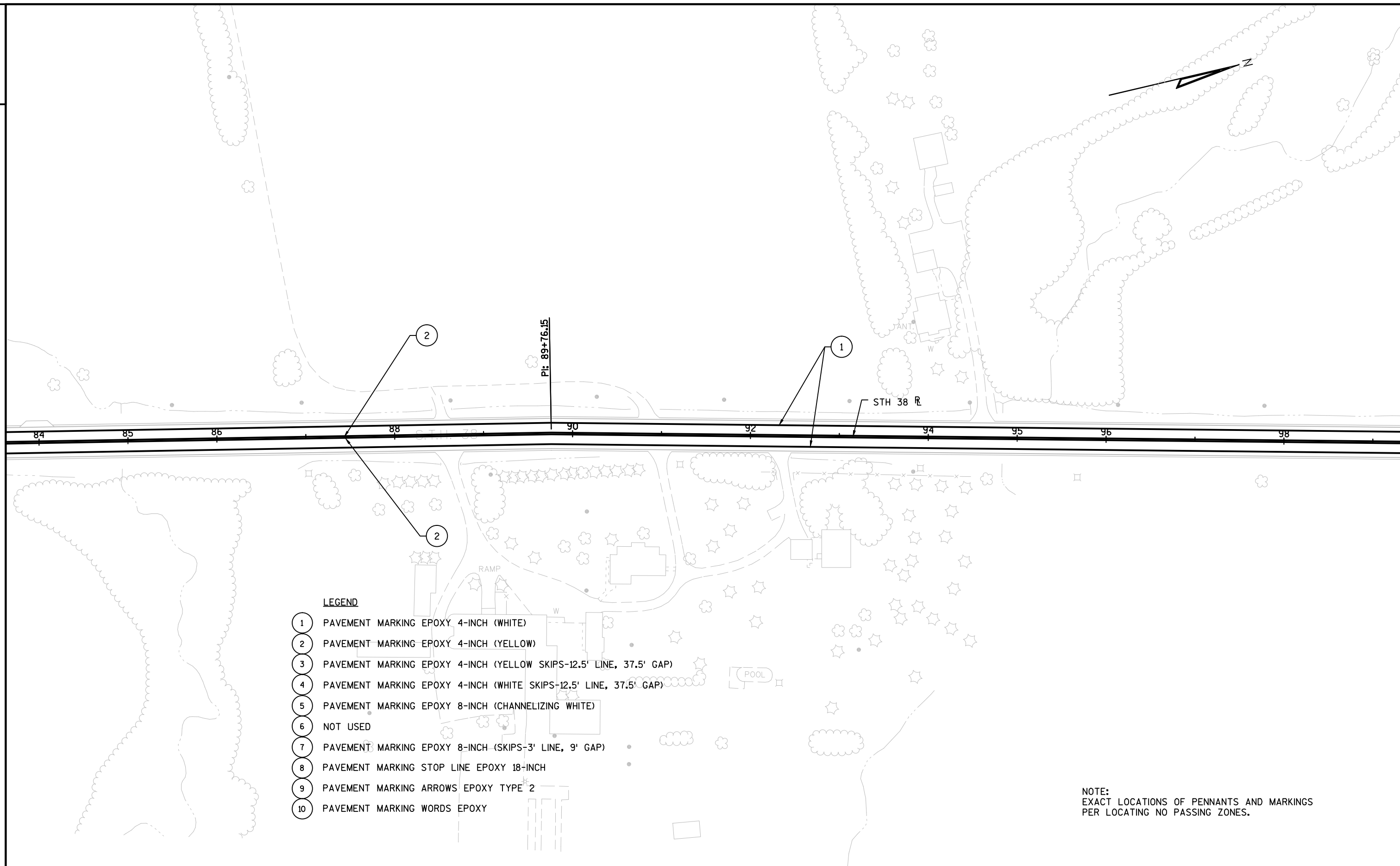


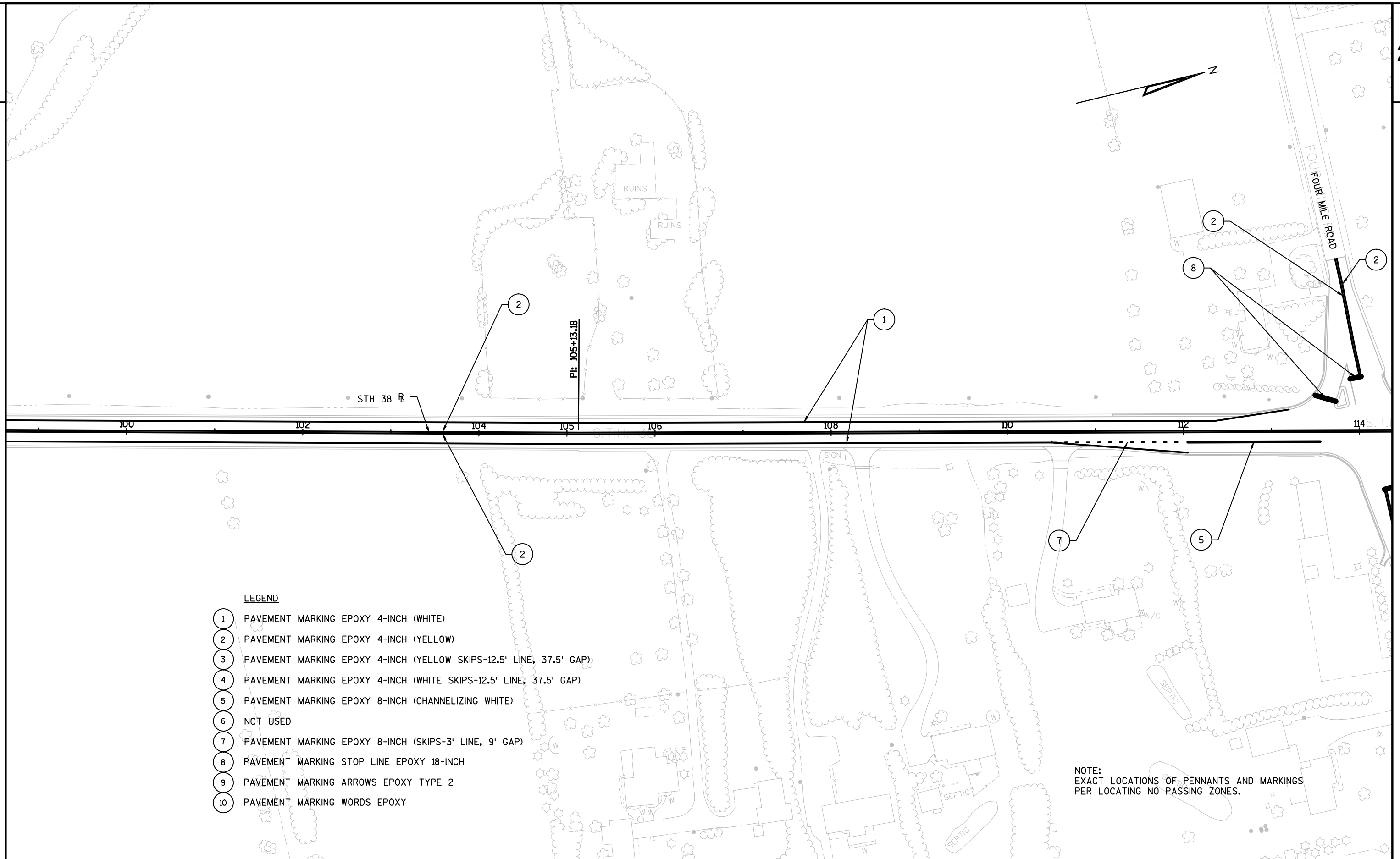


1	PAVEMENT MARKING EPOXY 4-INCH (WHITE)
2	PAVEMENT MARKING EPOXY 4-INCH (YELLOW)
3	PAVEMENT MARKING EPOXY 4-INCH (YELLOW SKIPS-12.5' LINE, 37.5' GAP)
4	PAVEMENT MARKING EPOXY 4-INCH (WHITE SKIPS-12.5' LINE, 37.5' GAP)
5	PAVEMENT MARKING EPOXY 8-INCH (CHANNELIZING WHITE)
6	NOT USED
7	PAVEMENT MARKING EPOXY 8-INCH (SKIPS-3' LINE, 9' GAP)
8	PAVEMENT MARKING STOP LINE EPOXY 18-INCH
9	PAVEMENT MARKING ARROWS EPOXY TYPE 2
10	PAVEMENT MARKING WORDS EPOXY

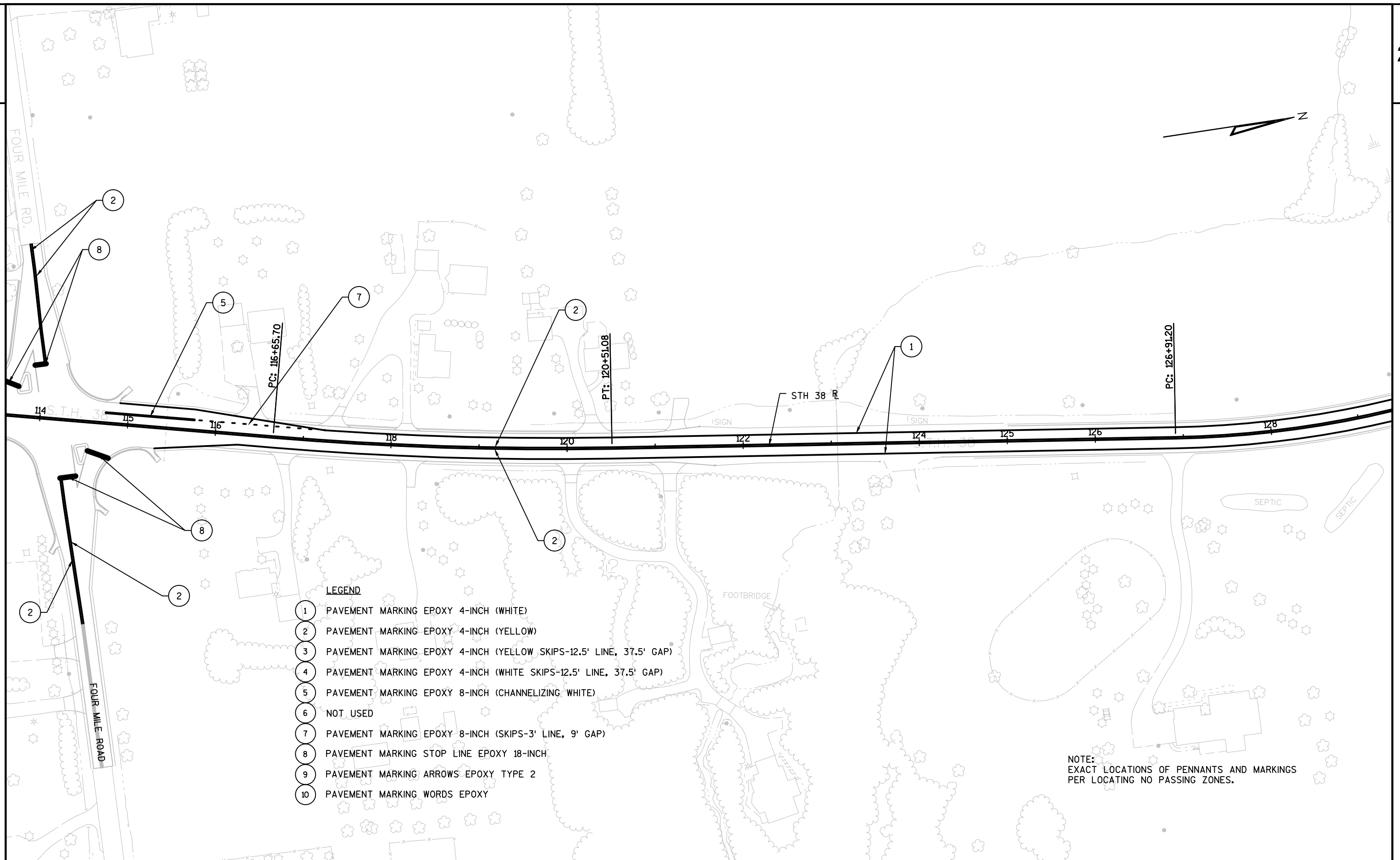
EXACT LOCATIONS OF PENNANTS AND MARKINGS
PER LOCATING NO-PASSING ZONES.

**E**



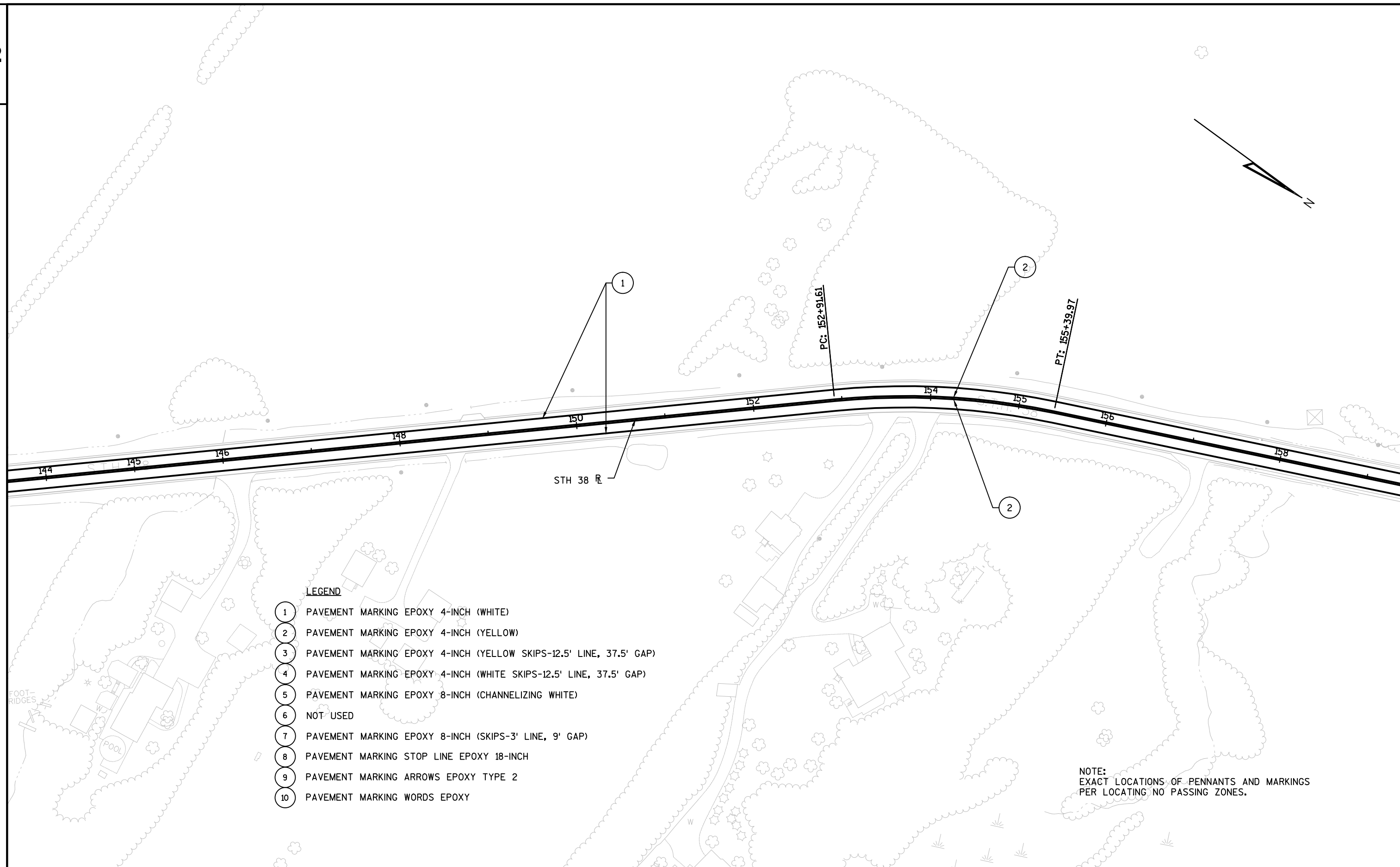


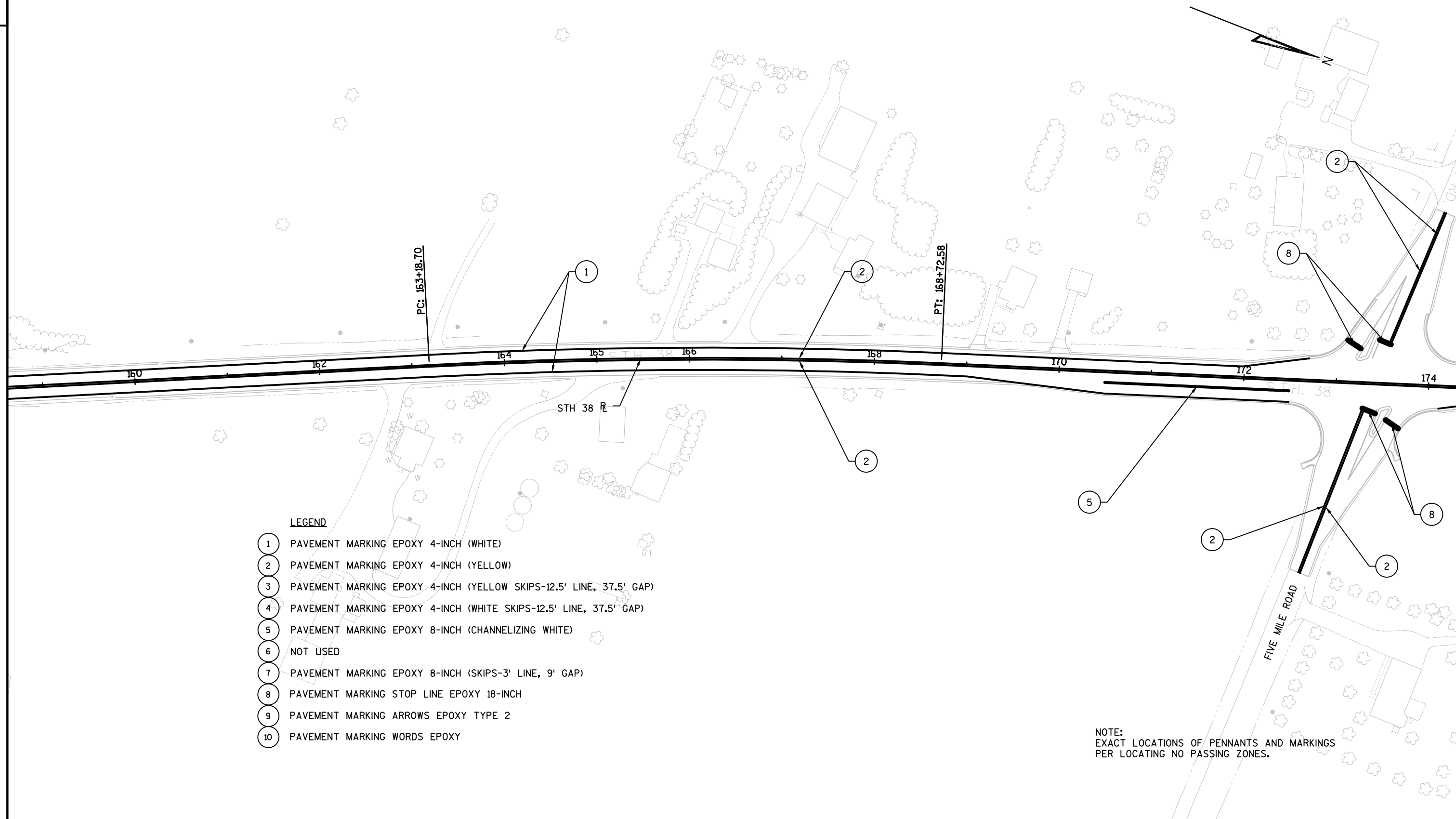
PROJECT NO:2060-17-70	HWY:STH 38	COUNTY:RACINE	PAVEMENT MARKING	SHEET	E
-----------------------	------------	---------------	------------------	-------	---

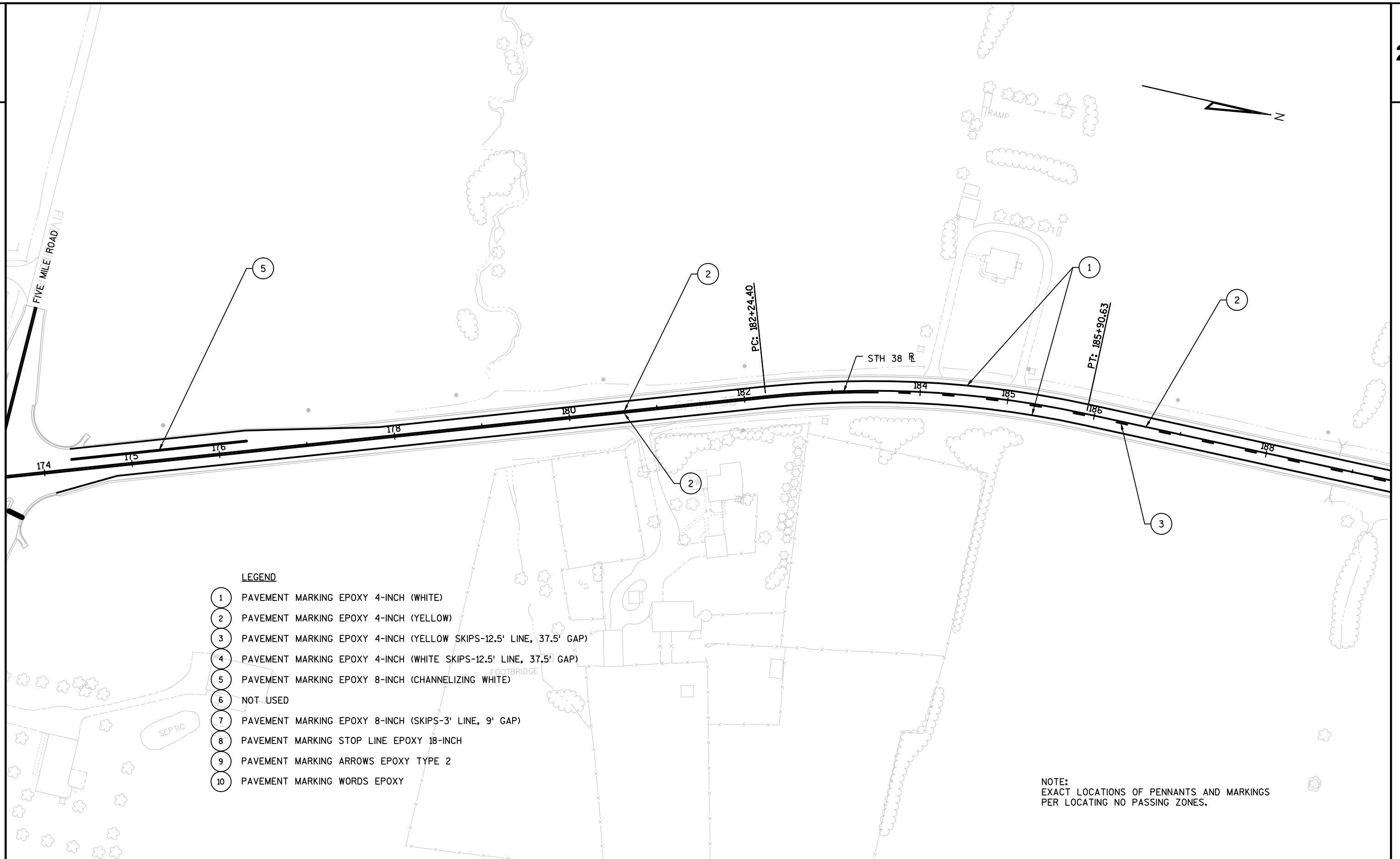


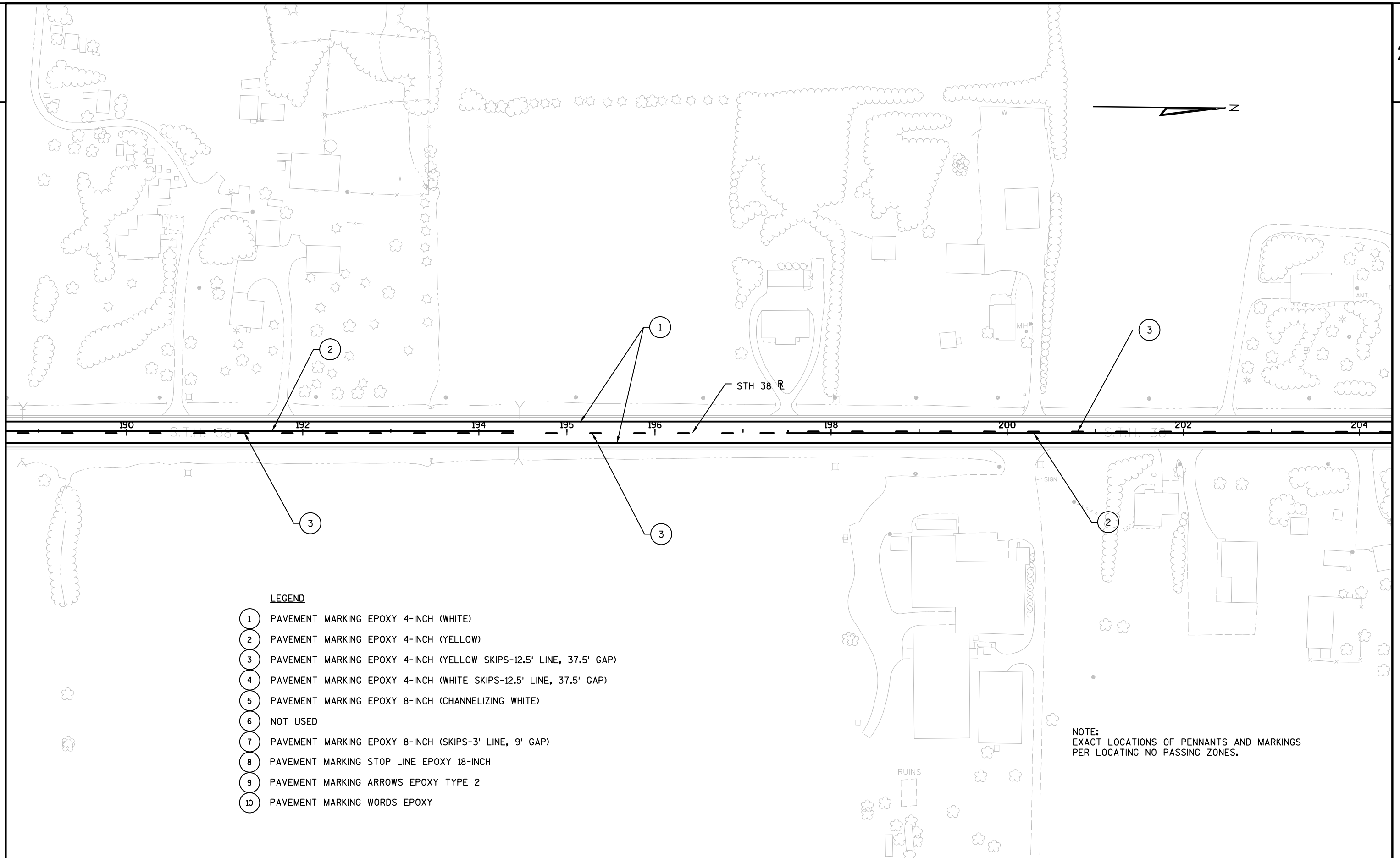


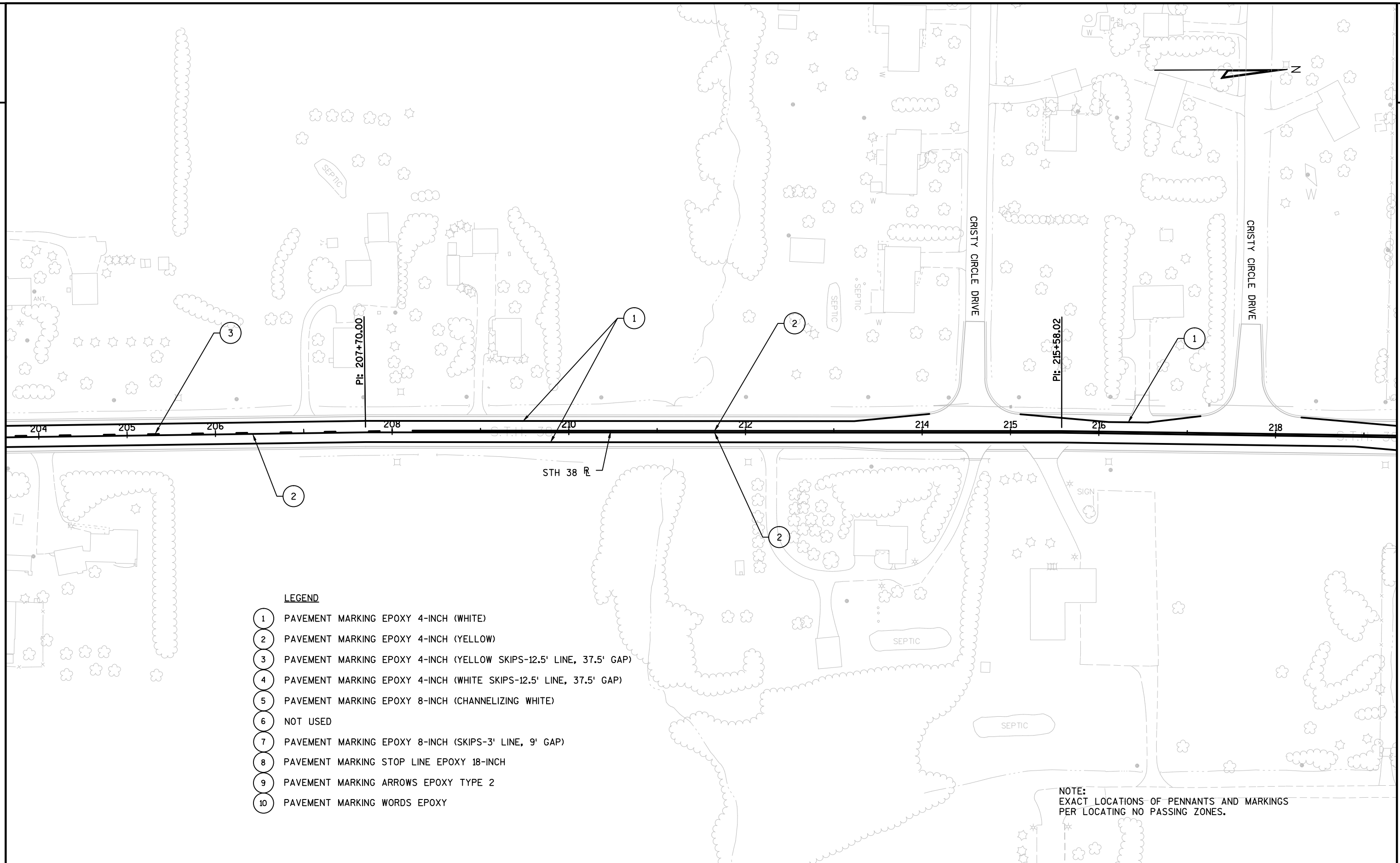
FILE NAME : N:\PDS\C3D\CAD\20601700\24500-PM.DWG	PLOT DATE : 1/29/2016 1:24 PM	PLOT BY : FERK, ANDREW C	PLOT NAME :	PLOT SCALE : 1 IN:100 FT	WISDOT/CADDs SHEET 42
LAYOUT NAME - 024500-PM - (08)_PM					







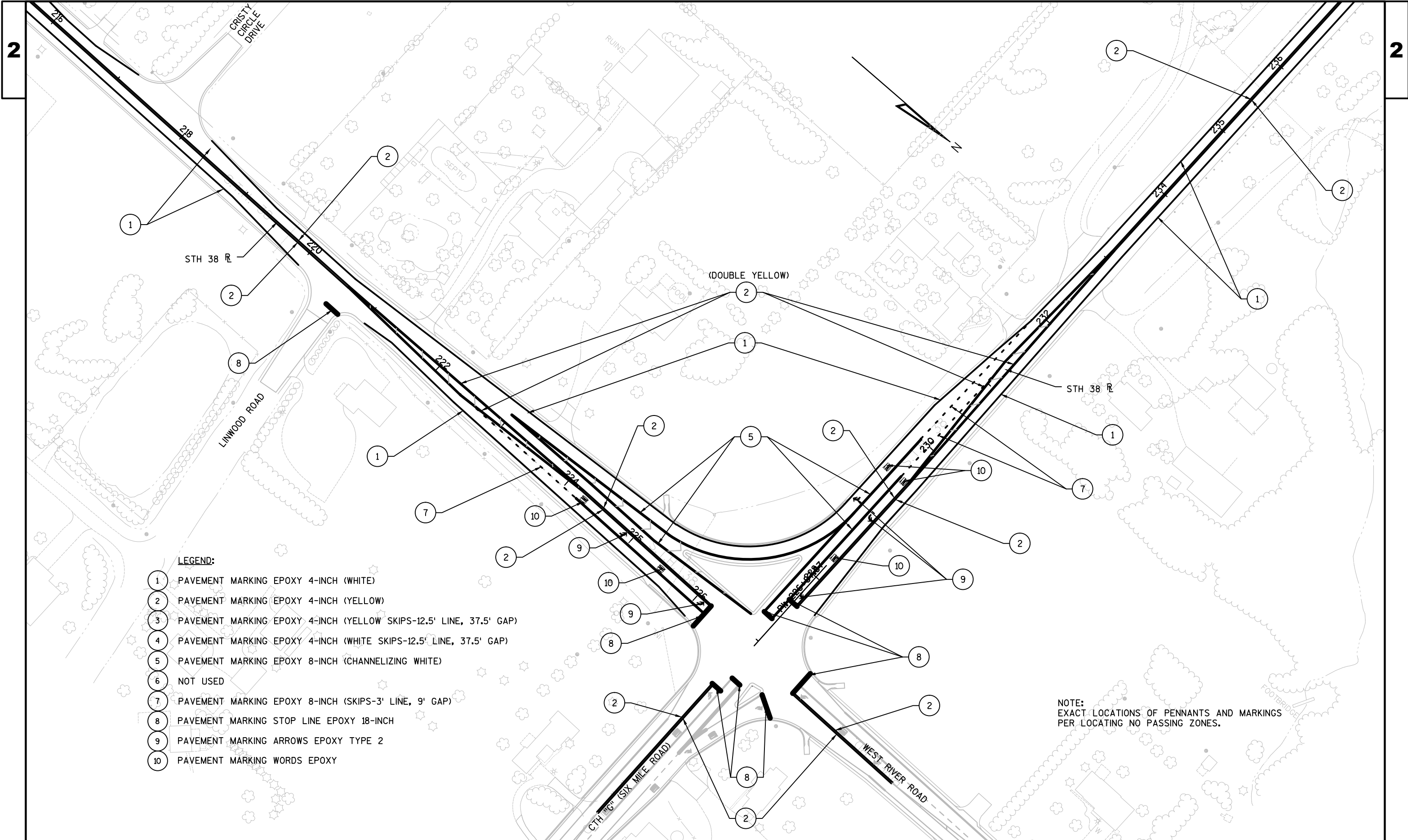




LEGEND

- 1 PAVEMENT MARKING EPOXY 4-INCH (WHITE)
- 2 PAVEMENT MARKING EPOXY 4-INCH (YELLOW)
- 3 PAVEMENT MARKING EPOXY 4-INCH (YELLOW SKIPS-12.5' LINE, 37.5' GAP)
- 4 PAVEMENT MARKING EPOXY 4-INCH (WHITE SKIPS-12.5' LINE, 37.5' GAP)
- 5 PAVEMENT MARKING EPOXY 8-INCH (CHANNELIZING WHITE)
- 6 NOT USED
- 7 PAVEMENT MARKING EPOXY 8-INCH (SKIPS-3' LINE, 9' GAP)
- 8 PAVEMENT MARKING STOP LINE EPOXY 18-INCH
- 9 PAVEMENT MARKING ARROWS EPOXY TYPE 2
- 10 PAVEMENT MARKING WORDS EPOXY

NOTE:
EXACT LOCATIONS OF PENNANTS AND MARKINGS
PER LOCATING NO PASSING ZONES.



PROJECT NO:2060-17-70

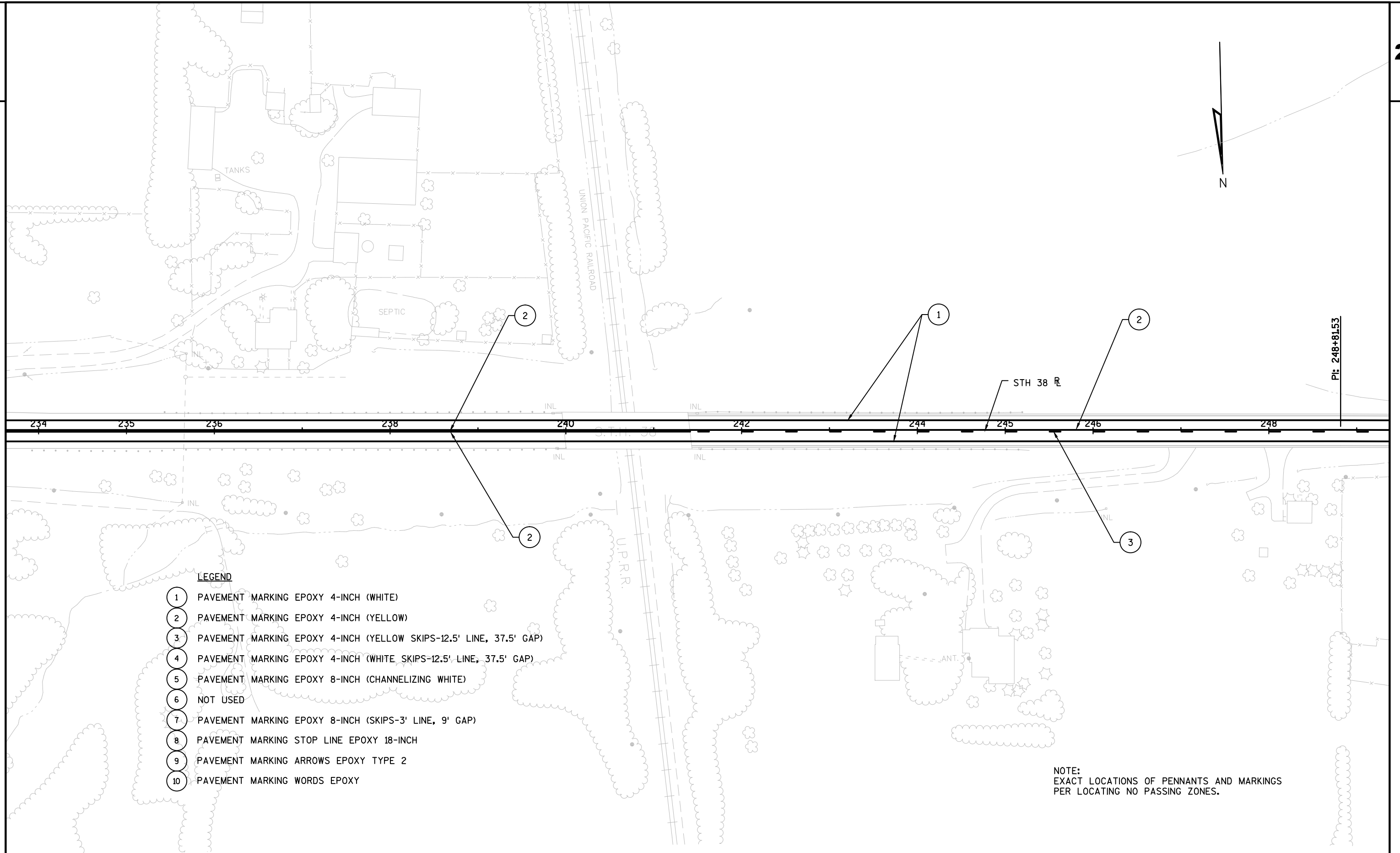
HWY:STH 38

COUNTY:RACINE

PAVEMENT MARKING

SHEET

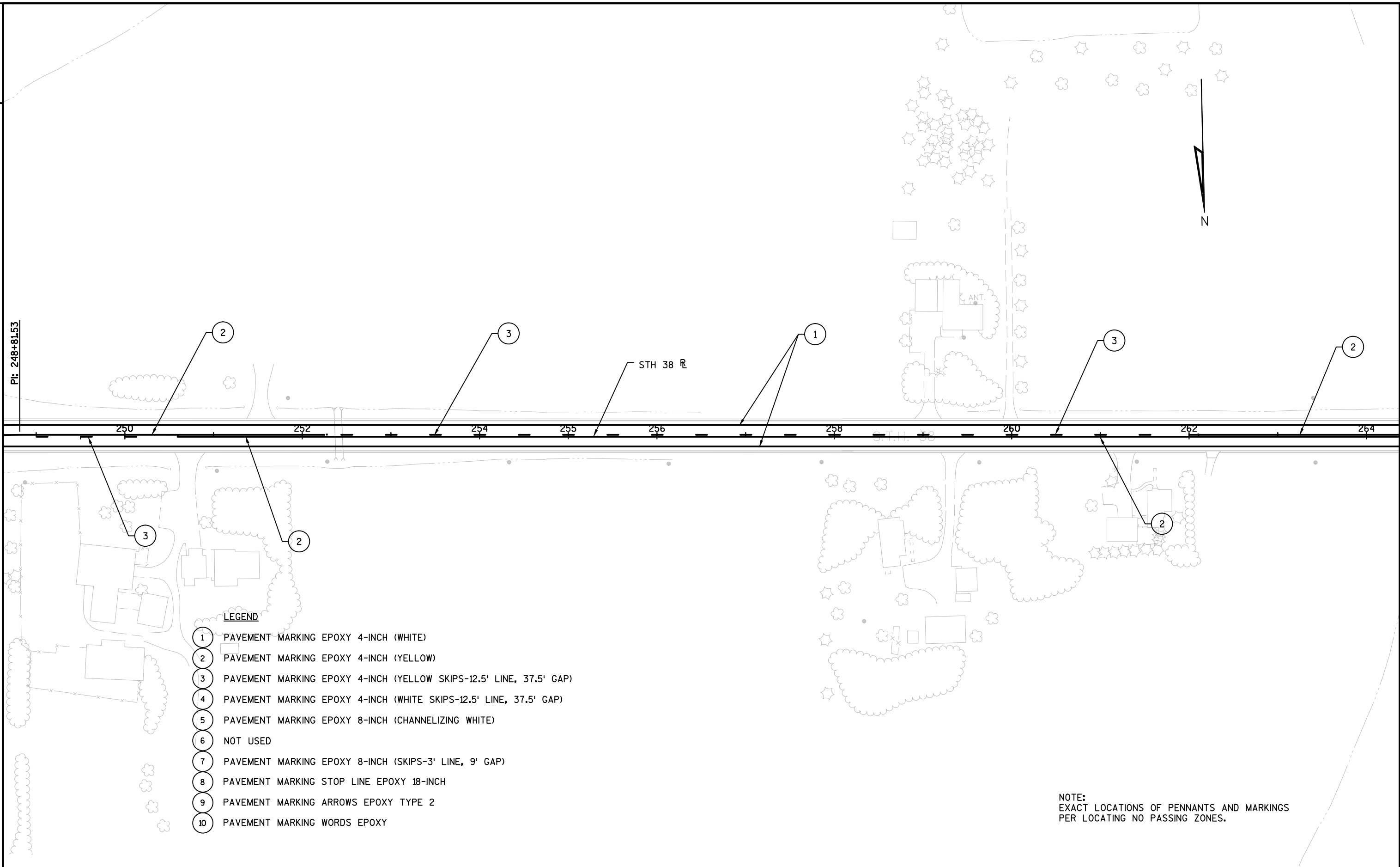
E



LEGEND

- 1 PAVEMENT MARKING EPOXY 4-INCH (WHITE)
- 2 PAVEMENT MARKING EPOXY 4-INCH (YELLOW)
- 3 PAVEMENT MARKING EPOXY 4-INCH (YELLOW SKIPS-12.5' LINE, 37.5' GAP)
- 4 PAVEMENT MARKING EPOXY 4-INCH (WHITE SKIPS-12.5' LINE, 37.5' GAP)
- 5 PAVEMENT MARKING EPOXY 8-INCH (CHANNELIZING WHITE)
- 6 NOT USED
- 7 PAVEMENT MARKING EPOXY 8-INCH (SKIPS-3' LINE, 9' GAP)
- 8 PAVEMENT MARKING STOP LINE EPOXY 18-INCH
- 9 PAVEMENT MARKING ARROWS EPOXY TYPE 2
- 10 PAVEMENT MARKING WORDS EPOXY

NOTE:
EXACT LOCATIONS OF PENNANTS AND MARKINGS
PER LOCATING NO PASSING ZONES.



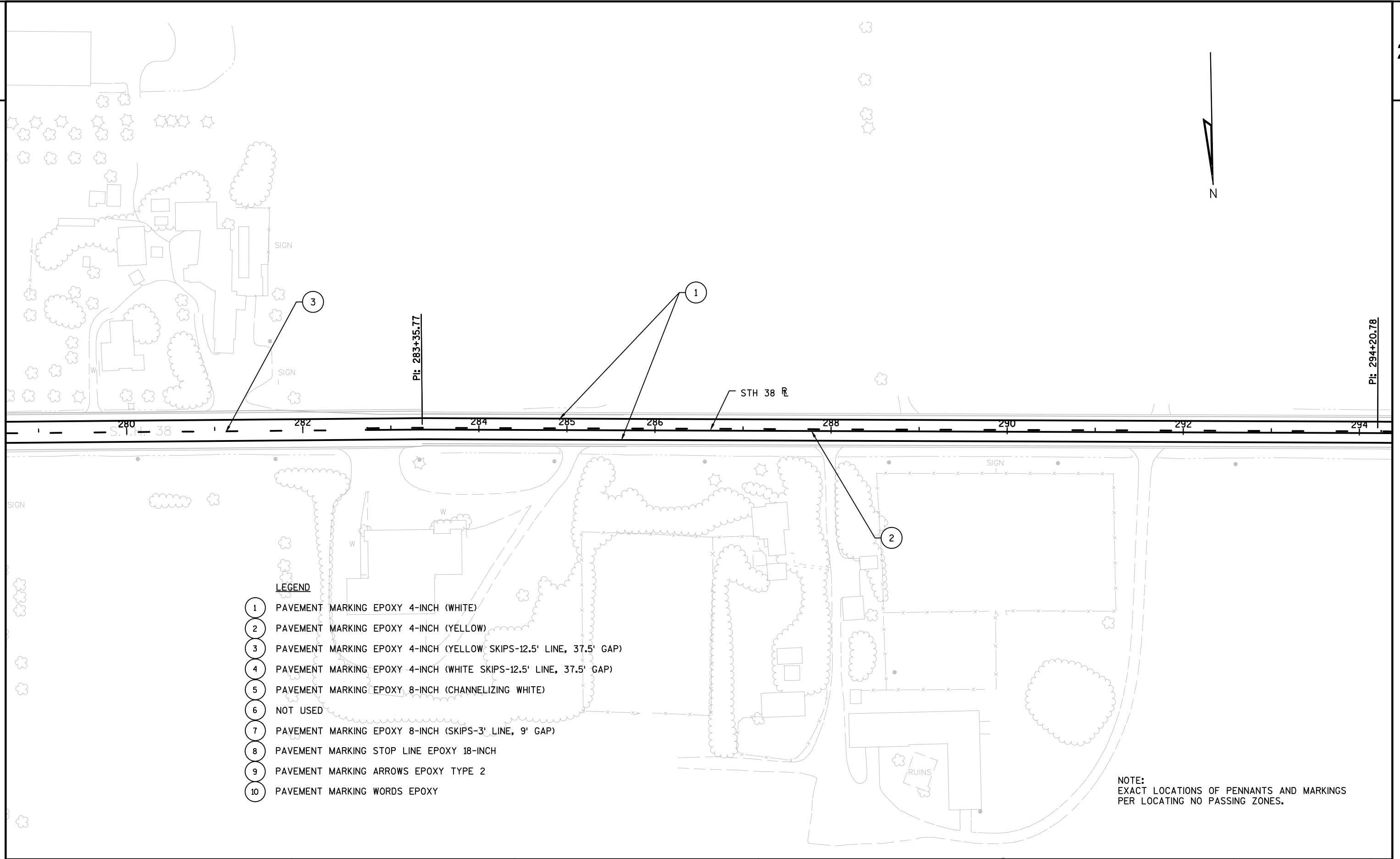
- LEGEND**
- 1 PAVEMENT MARKING EPOXY 4-INCH (WHITE)
 - 2 PAVEMENT MARKING EPOXY 4-INCH (YELLOW)
 - 3 PAVEMENT MARKING EPOXY 4-INCH (YELLOW SKIPS-12.5' LINE, 37.5' GAP)
 - 4 PAVEMENT MARKING EPOXY 4-INCH (WHITE SKIPS-12.5' LINE, 37.5' GAP)
 - 5 PAVEMENT MARKING EPOXY 8-INCH (CHANNELIZING WHITE)
 - 6 NOT USED
 - 7 PAVEMENT MARKING EPOXY 8-INCH (SKIPS-3' LINE, 9' GAP)
 - 8 PAVEMENT MARKING STOP LINE EPOXY 18-INCH
 - 9 PAVEMENT MARKING ARROWS EPOXY TYPE 2
 - 10 PAVEMENT MARKING WORDS EPOXY

NOTE:
EXACT LOCATIONS OF PENNANTS AND MARKINGS
PER LOCATING NO PASSING ZONES.

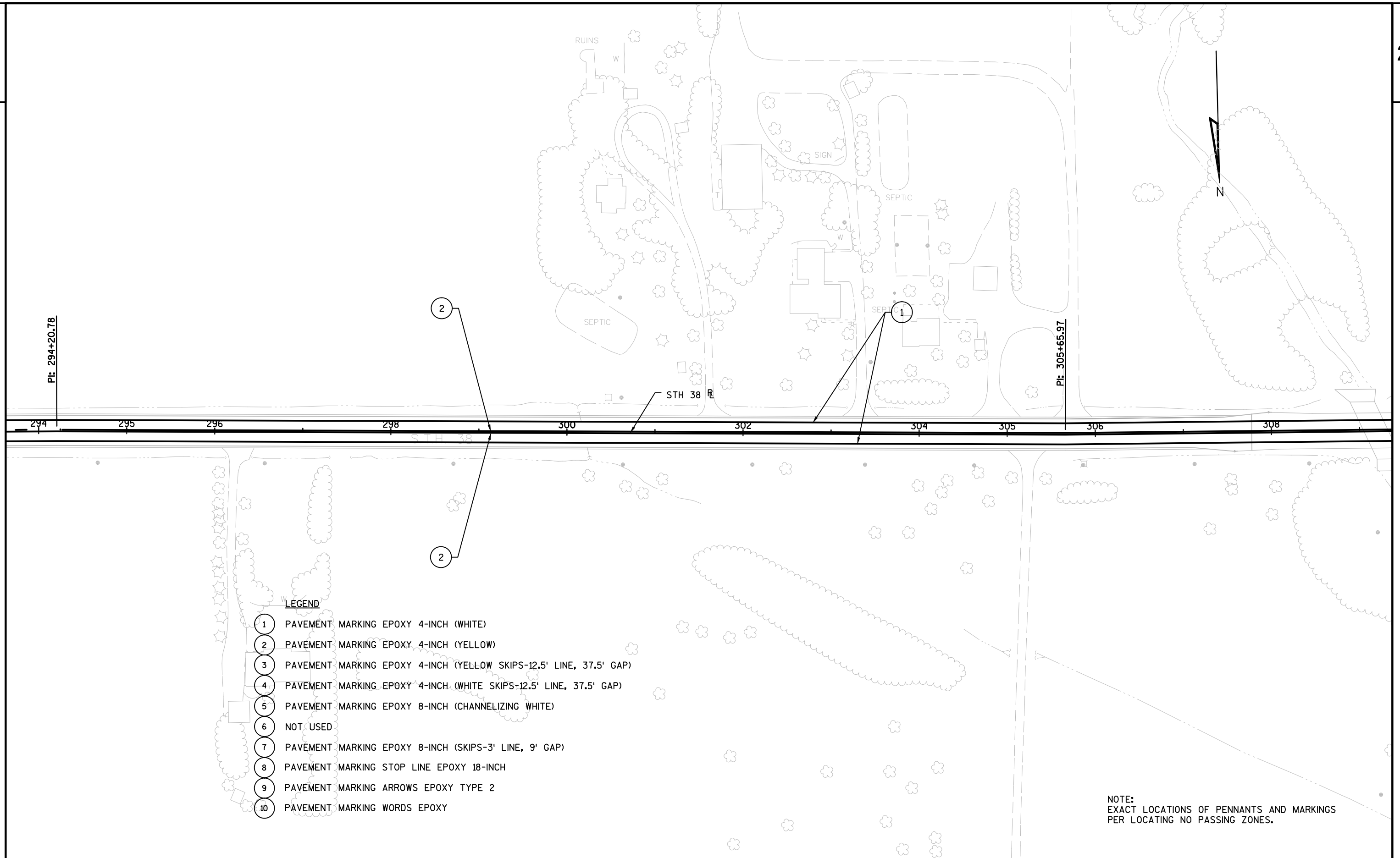


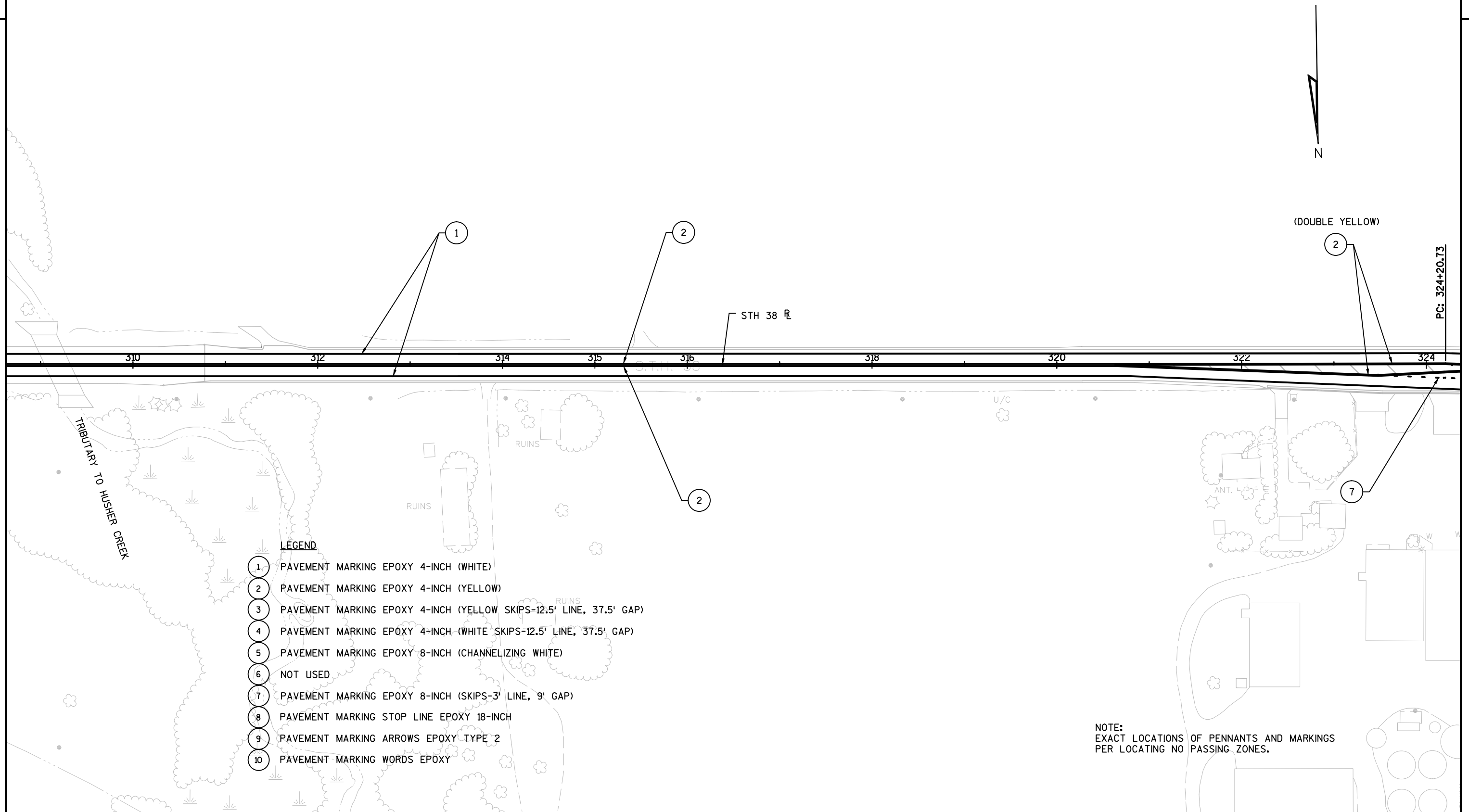
2

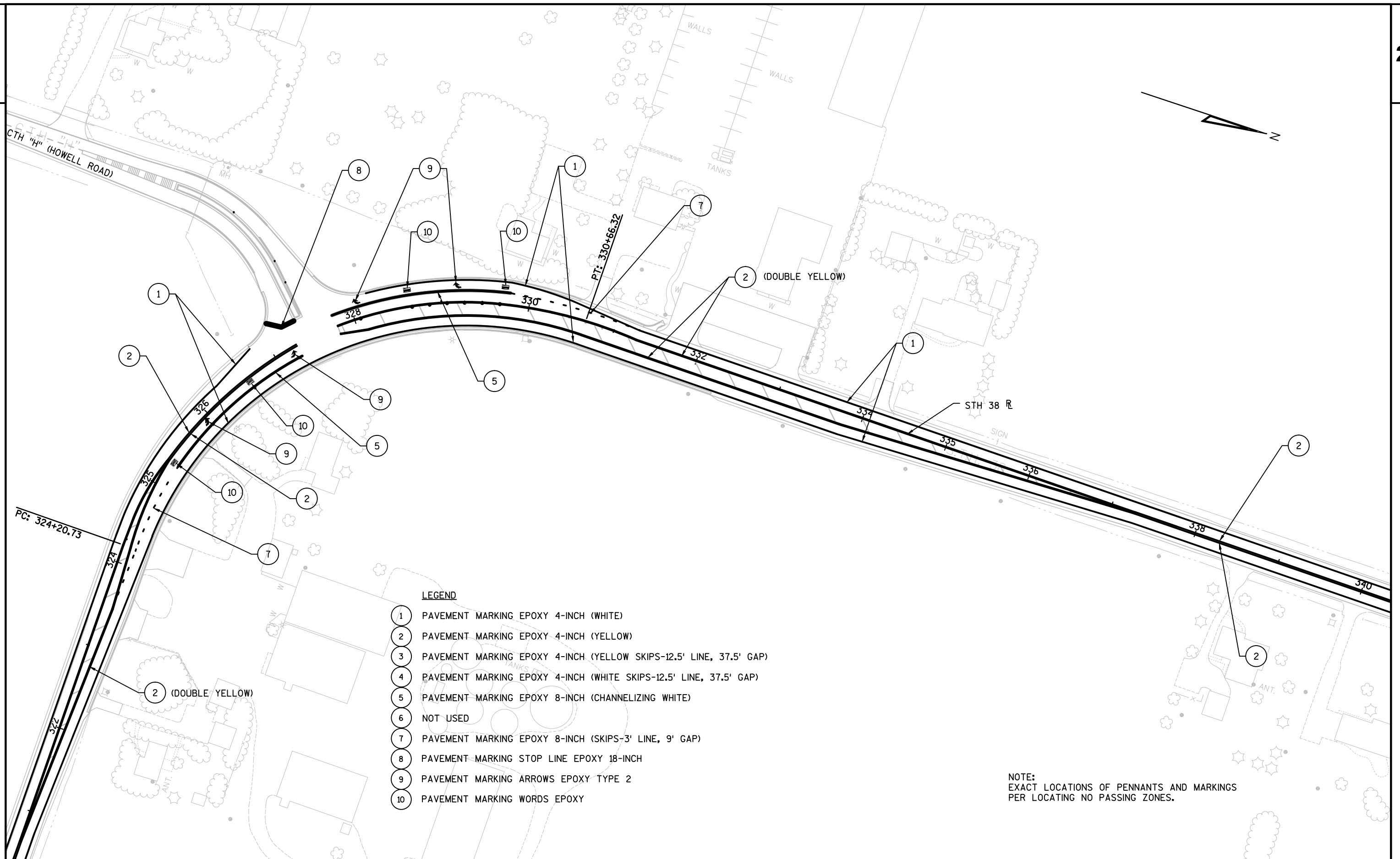
2

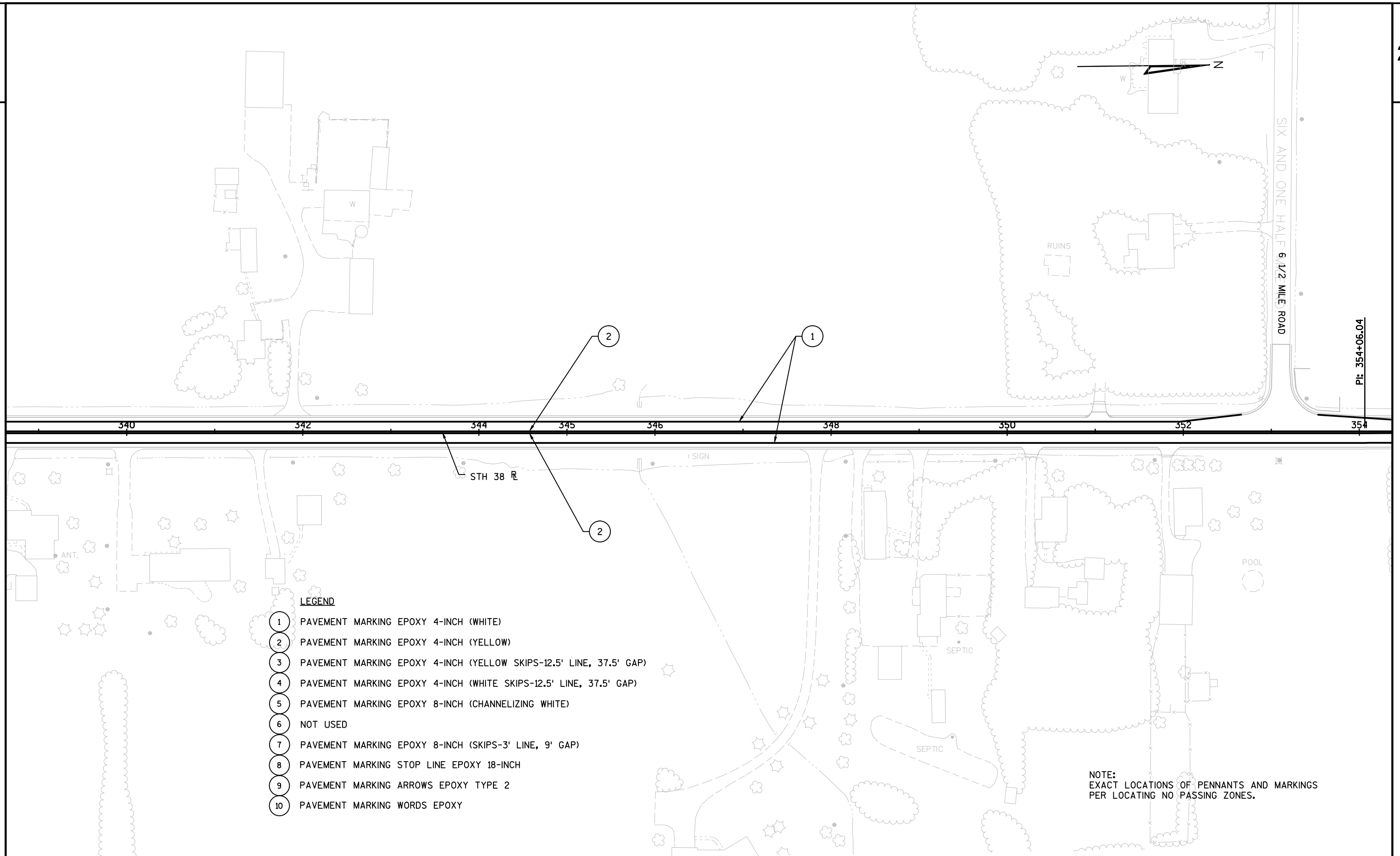


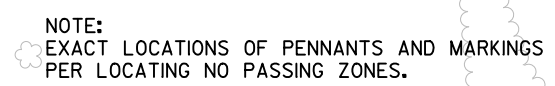
PROJECT NO:2060-17-70	HWY:STH 38	COUNTY:RACINE	PAVEMENT MARKING	SHEET	E
-----------------------	------------	---------------	------------------	-------	----------



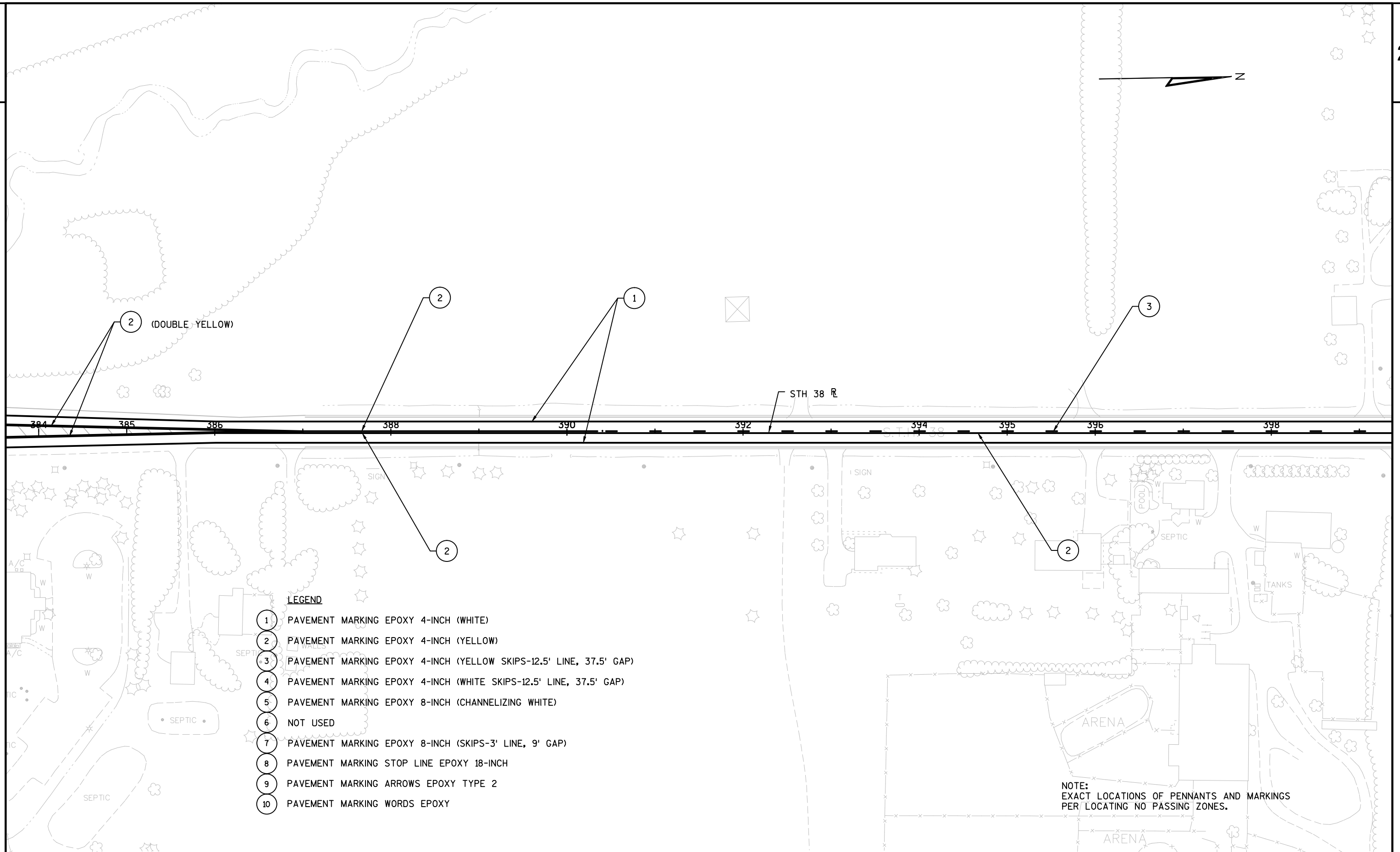


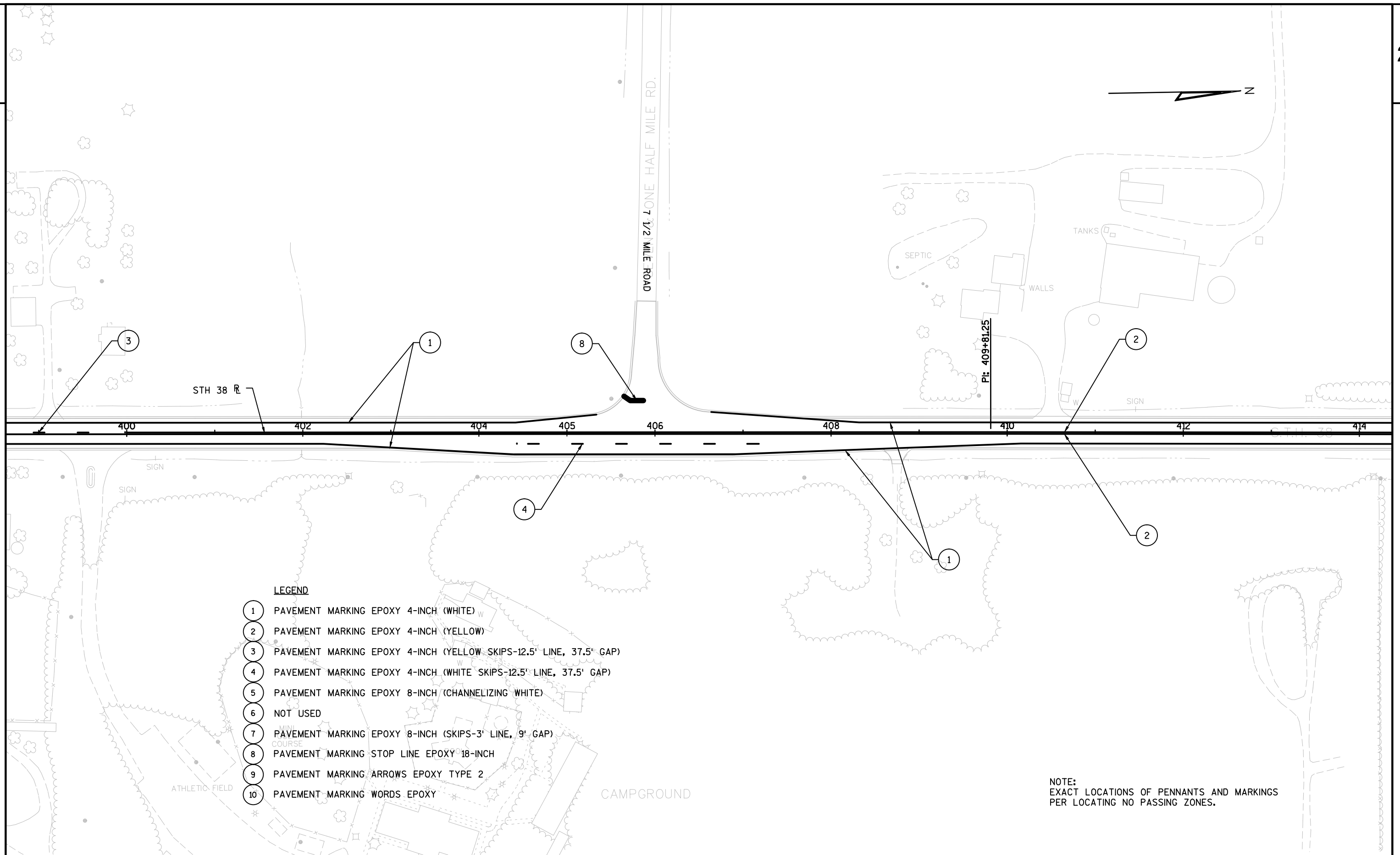


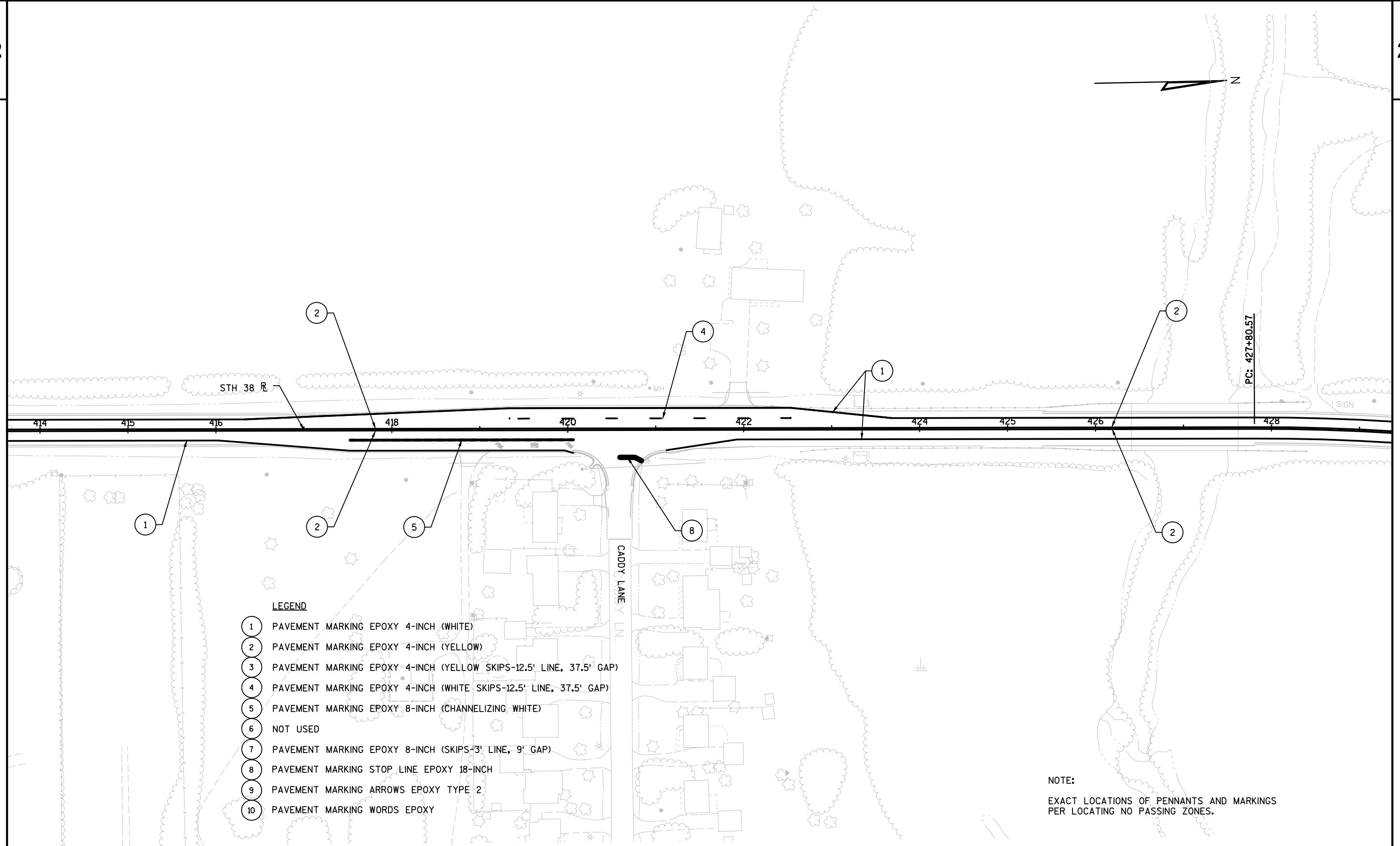


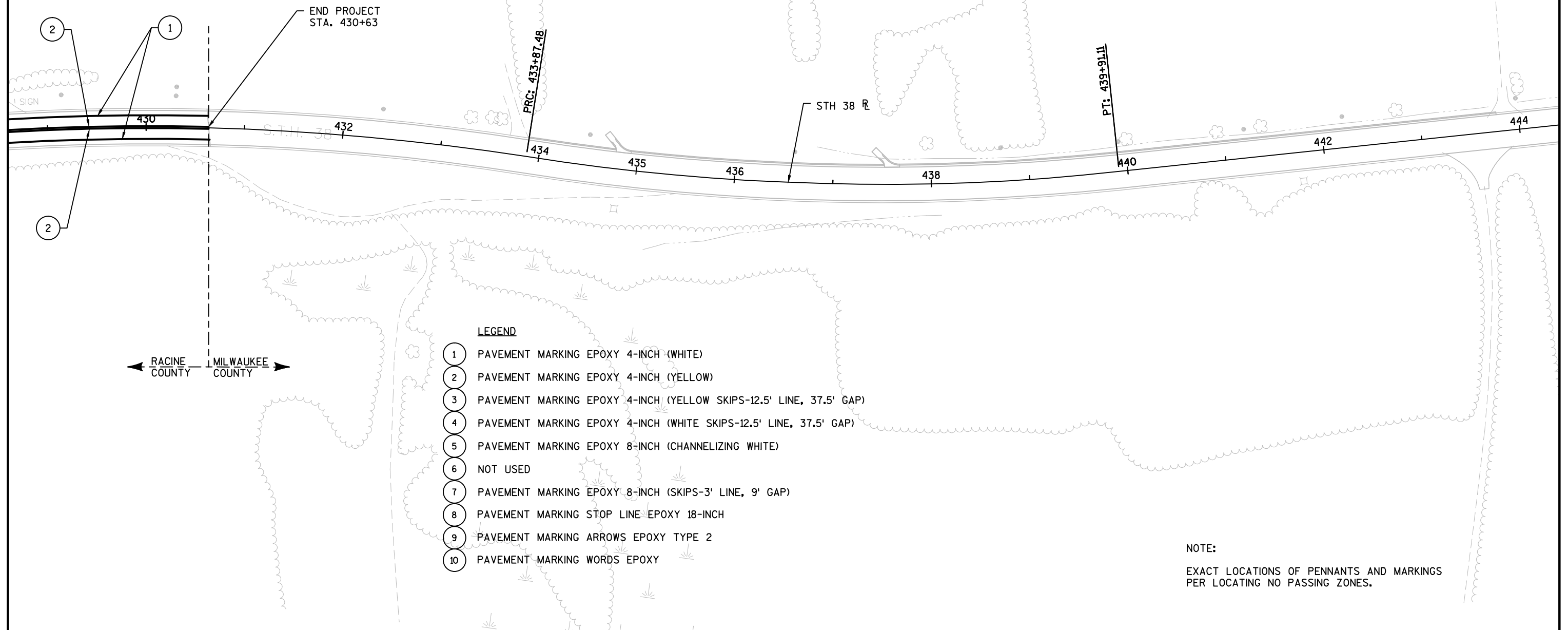












LEGEND -- APPLY THE DRAWINGS LISTED BELOW TO THE GENERAL LOCATIONS SHOWN ON THE MAP.

STANDARD DETAIL DRAWINGS (SDD's)

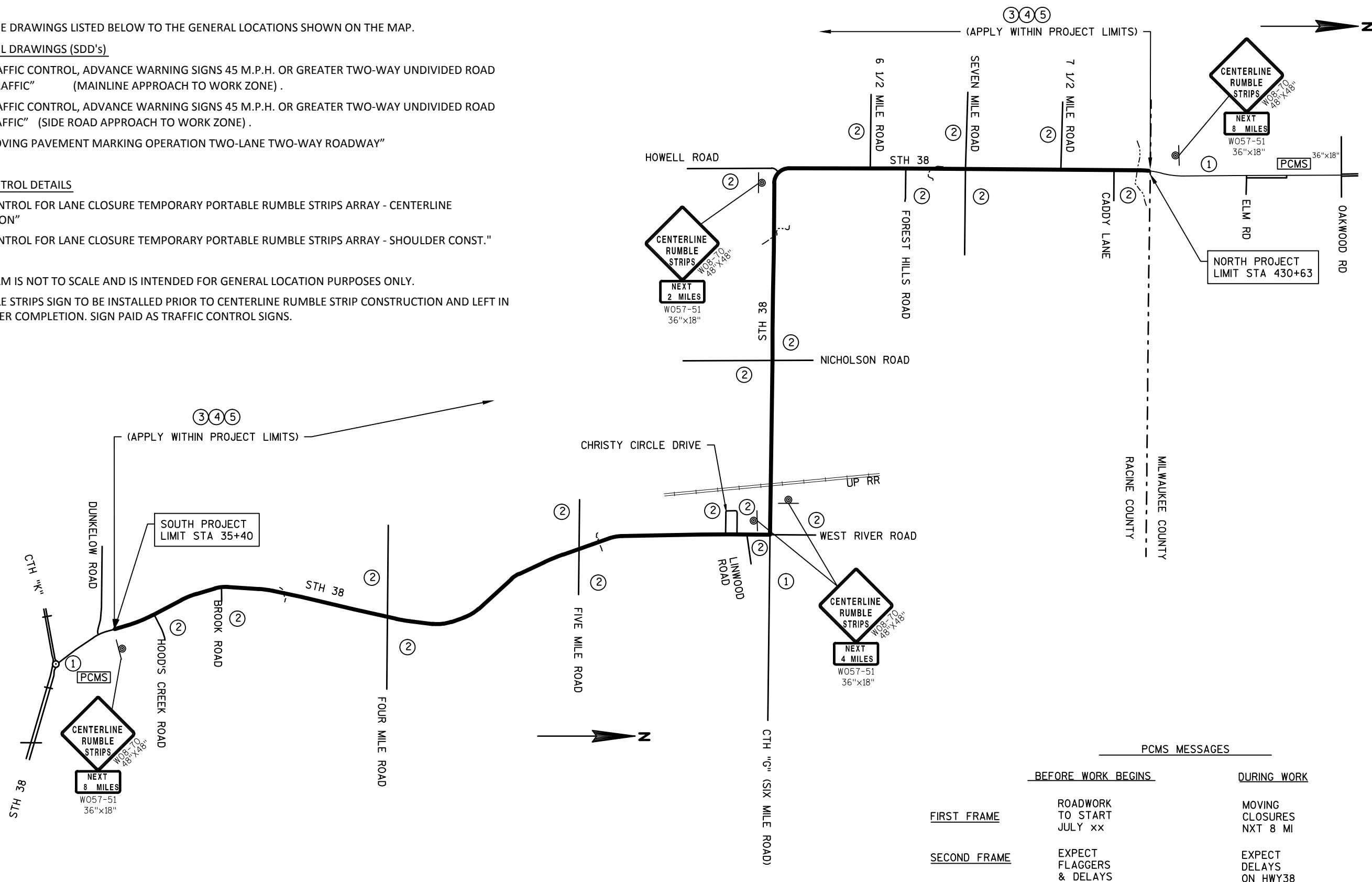
- ① SDD for "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC" (MAINLINE APPROACH TO WORK ZONE) .
- ② SDD for "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC" (SIDE ROAD APPROACH TO WORK ZONE) .
- ③ SDD for "MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY"

TRAFFIC CONTROL DETAILS

- ④ "TRAFFIC CONTROL FOR LANE CLOSURE TEMPORARY PORTABLE RUMBLE STRIPS ARRAY - CENTERLINE CONSTRUCTION"
- ⑤ "TRAFFIC CONTROL FOR LANE CLOSURE TEMPORARY PORTABLE RUMBLE STRIPS ARRAY - SHOULDER CONST."

NOTE: THIS DIAGRAM IS NOT TO SCALE AND IS INTENDED FOR GENERAL LOCATION PURPOSES ONLY.

CENTERLINE RUMBLE STRIPS SIGN TO BE INSTALLED PRIOR TO CENTERLINE RUMBLE STRIP CONSTRUCTION AND LEFT IN PLACE 15 DAYS AFTER COMPLETION. SIGN PAID AS TRAFFIC CONTROL SIGNS.



PCMS MESSAGES

	BEFORE WORK BEGINS	DURING WORK
FIRST FRAME	ROADWORK TO START JULY xx	MOVING CLOSURES NXT 8 MI
SECOND FRAME	EXPECT FLAGGERS & DELAYS	EXPECT DELAYS ON HWY38

NOTE:

VERIFY PCMS LOCATIONS AND MESSAGES WITH ENGINEER PRIOR TO DEPLOYMENT.
DEPLOY PCMS 7 CALENDAR DAYS BEFORE WORK BEGINS.

LEGEND

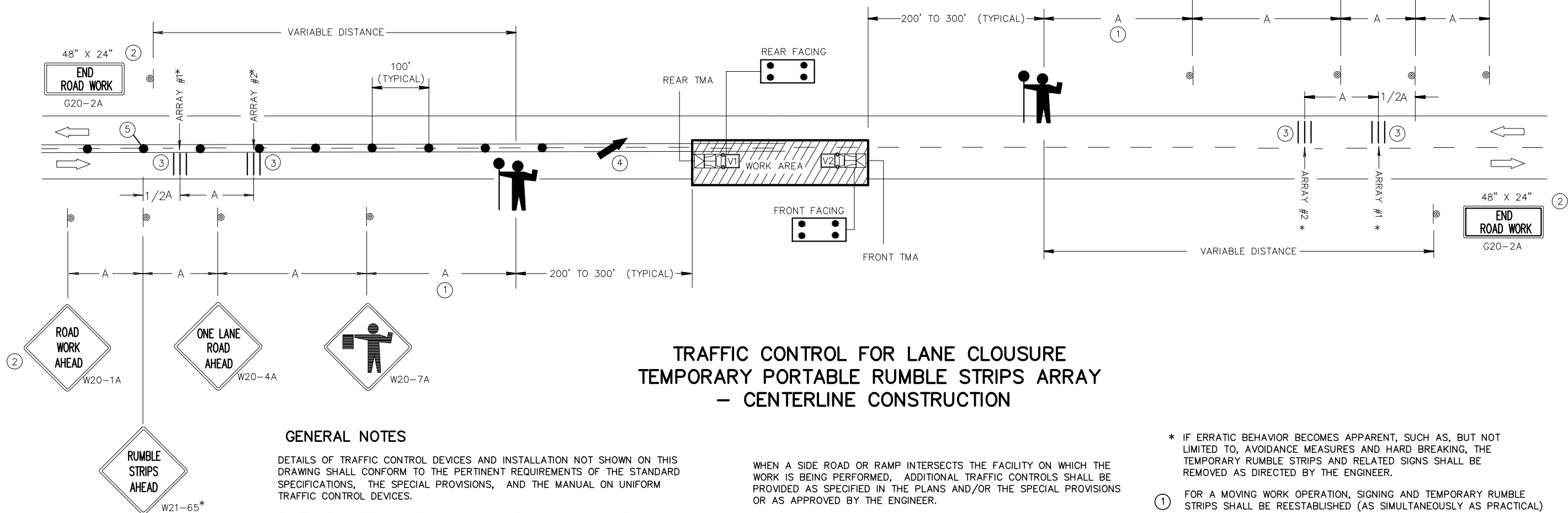
- ⊙ SIGN ON PORTABLE OR PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA
- 🚧 FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF
- TRAFFIC CONTROL DRUMS

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING A.



TRAFFIC CONTROL FOR LANE CLOSURE TEMPORARY PORTABLE RUMBLE STRIPS ARRAY - CENTERLINE CONSTRUCTION

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE TRUCK-MOUNTED ATTENUATORS (TMA's) SHALL BE USED FOR MOVING OPERATIONS ONLY.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ADDITIONAL WORK VEHICLES MAY BE USED BETWEEN V1 AND V2.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE AND OTHER FACTORS.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS. PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* IF ERRATIC BEHAVIOR BECOMES APPARENT, SUCH AS, BUT NOT LIMITED TO, AVOIDANCE MEASURES AND HARD BREAKING, THE TEMPORARY RUMBLE STRIPS AND RELATED SIGNS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

- FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- EACH RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED 8 - 10 FEET CENTER TO CENTER, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- PROVIDE AN EVEN SURFACE WITH THE EXISTING PAVEMENT AS APPROVED BY THE ENGINEER TO PROTECT THE TRAVELLING VEHICLES FROM DAMAGE AND TO PROVIDE PROTECTION OF THE MILLED SURFACE.
- AFTER MILLING FOR THE LONGITUDINAL JOINT REPAIR BUT PRIOR TO INSTALLING PAVEMENT MARKING ON THE NEW HMA SURFACE, THE CONTRACTOR SHALL DELINEATE THE CENTERLINE WITH DRUMS OR OTHER ENGINEER-APPROVED DEVICE. PAY AS TRAFFIC CONTROL DRUMS.

LEGEND

- ⊙ SIGN ON PORTABLE OR PERMANENT SUPPORT
- ➞ DIRECTION OF TRAFFIC
- ▨ WORK AREA
- 🚧 FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING A.



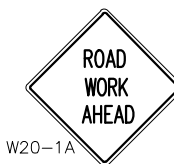
W21-65*



W20-7A

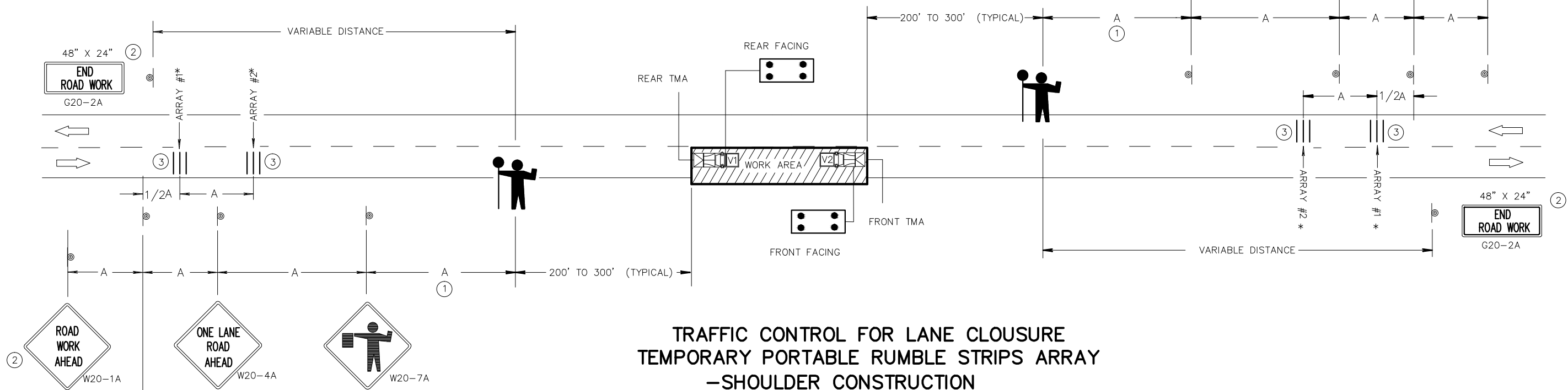


W20-4A



W20-1A

②



TRAFFIC CONTROL FOR LANE CLOSURE
TEMPORARY PORTABLE RUMBLE STRIPS ARRAY
-SHOULDER CONSTRUCTION

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE TRUCK-MOUNTED ATTENUATORS (TMA'S) SHALL BE USED FOR MOVING OPERATIONS ONLY.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ADDITIONAL WORK VEHICLES MAY BE USED BETWEEN V1 AND V2.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE AND OTHER FACTORS.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS. PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

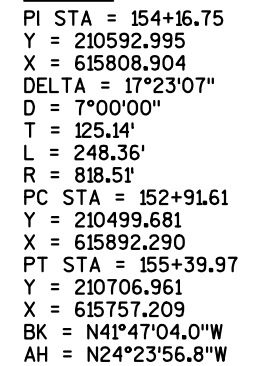
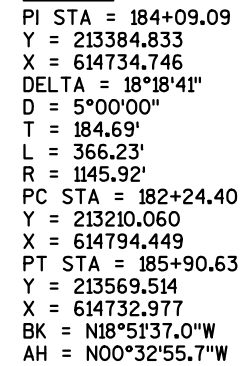
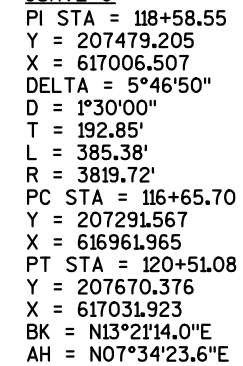
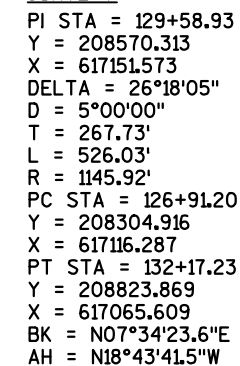
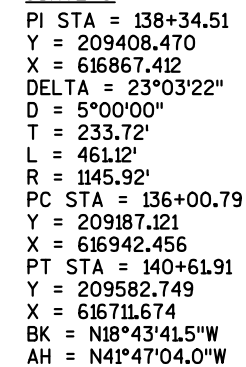
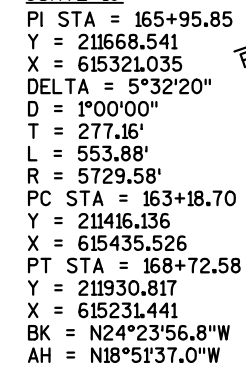
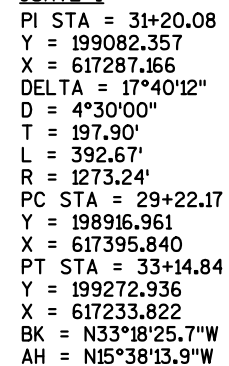
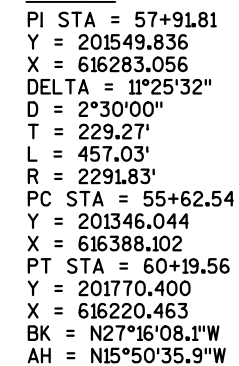
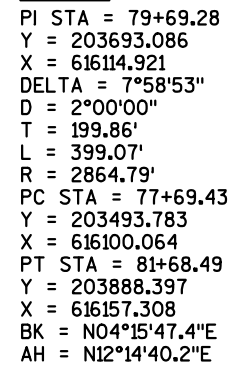
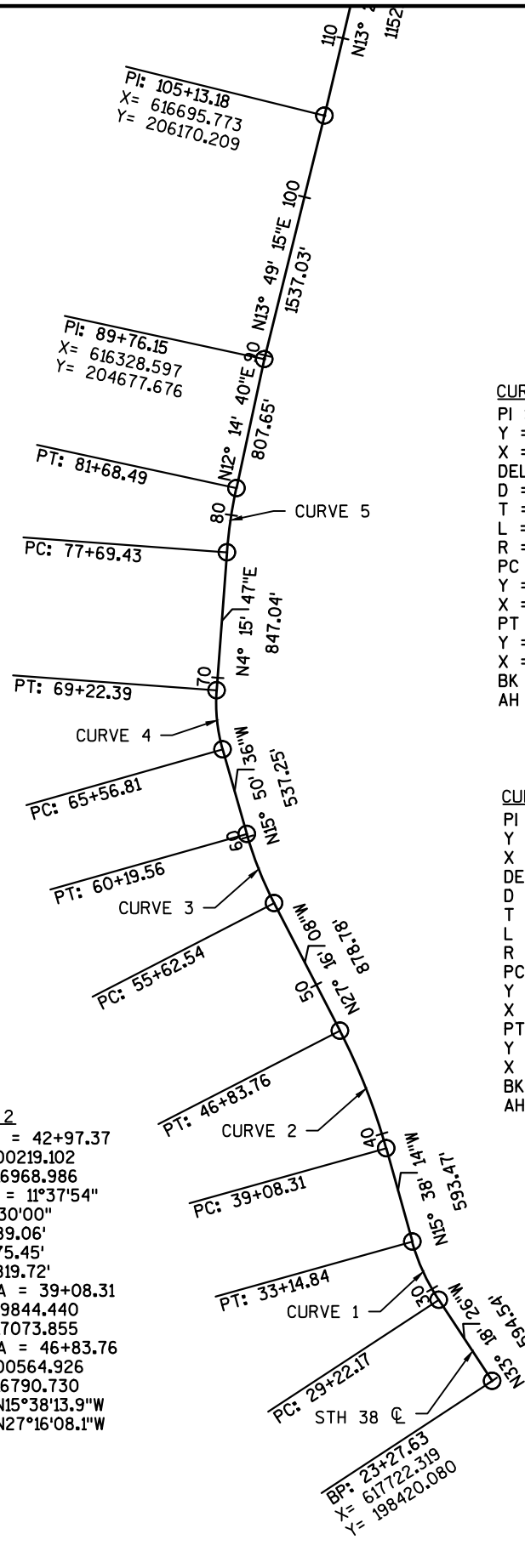
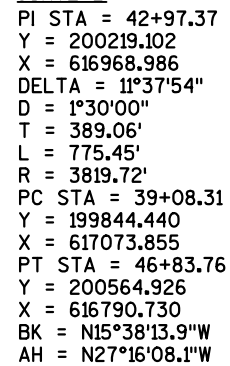
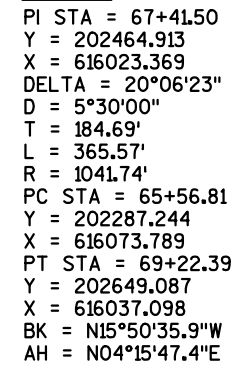
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, REMOVE TEMPORARY RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

* IF ERRATIC BEHAVIOR BECOMES APPARENT, SUCH AS, BUT NOT LIMITED TO, AVOIDANCE MEASURES AND HARD BREAKING, THE TEMPORARY RUMBLE STRIPS AND RELATED SIGNS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

① FOR A MOVING WORK OPERATION, SIGNING AND TEMPORARY RUMBLE STRIPS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3,500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

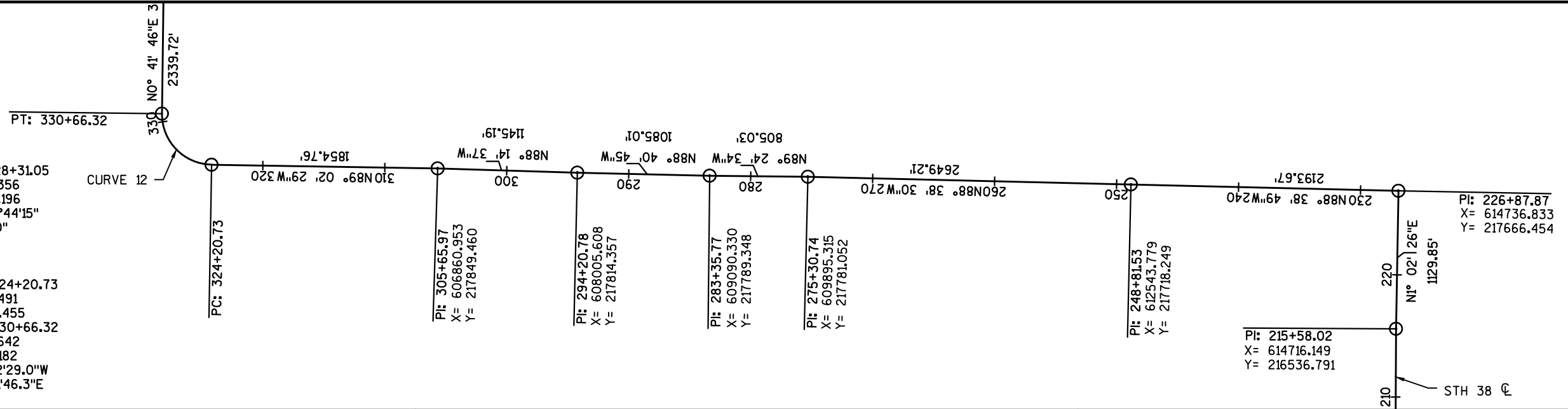
② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

③ EACH RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED 8 - 10 FEET CENTER TO CENTER, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.



CURVE 12

PI STA = 328+31.05
Y = 217887.356
X = 604596.196
DELTA = 89°44'15"
D = 13°54'00"
T = 410.32'
L = 645.59'
R = 412.20'
PC STA = 324+20.73
Y = 217880.491
X = 605006.455
PT STA = 330+66.32
Y = 218297.642
X = 604601.182
BK = N89°02'29.0"W
AH = N00°41'46.3"E



CURVE 14

PI STA = 436+91.02
Y = 228902.860
X = 604921.161
DELTA = 14°56'05"
D = 2°28'27"
T = 303.53'
L = 603.62'
R = 2315.74'
PC STA = 433+87.48
Y = 228609.895
X = 604841.763
PT STA = 439+91.11
Y = 229206.390
X = 604922.375
BK = N15°09'50.0"E
AH = N00°13'44.8"E

CURVE 13

PI STA = 430+85.56
Y = 228315.527
X = 604761.984
DELTA = 14°03'36"
D = 2°19'00"
T = 304.99'
L = 606.91'
R = 2473.20'
PC STA = 427+80.57
Y = 228010.597
X = 604756.109
PT STA = 433+87.48
Y = 228609.895
X = 604841.763
BK = N01°06'13.7"E
AH = N15°09'50.0"E

EP: 483+66.39
X= 604944.931
Y= 233581.614

PI: 457+21.40
X= 604929.294
Y= 230936.672

DATE 01MAR16		E S T I M A T E O F Q U A N T I T I E S			
LINE					2060-17-70
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTI TY
0010	204.0125	Removing Asphalt ic Surface Milling	TON	777.000	777.000
0020	213.0100	Finishing Roadway (project) 01. 2060-17-70	EACH	1.000	1.000
0030	305.0110	Base Aggregate Dense 3/4-Inch	TON	5.000	5.000
0040	455.0605	Tack Coat	GAL	429.000	429.000
0050	460.2000	Incentive Densi ty HMA Pavement	DOL	510.000	510.000
0060	460.6224	HMA Pavement 4 MT 58-28 S	TON	777.000	777.000
0070	465.0425	Asphalt ic Shoulder Rumble Strips 2-Lane Rural	LF	48,684.000	48,684.000
0080	465.0475	Asphalt Center Line Rumble Strips 2-Lane Rural	LF	33,770.000	33,770.000
0090	619.1000	Mobilization	EACH	1.000	1.000
0100	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	8.000	8.000
0110	638.2102	Moving Signs Type II	EACH	8.000	8.000
0120	638.3000	Removing Small Sign Supports	EACH	8.000	8.000
0130	642.5001	Field Office Type B	EACH	1.000	1.000
0140	643.0100	Traffic Control (project) 01. 2060-17-70	EACH	1.000	1.000
0150	643.0300	Traffic Control Drums	DAY	450.000	450.000
0160	643.0900	Traffic Control Signs	DAY	1,400.000	1,400.000
0170	643.1050	Traffic Control Signs PCMS	DAY	42.000	42.000
0180	643.1055.S	Truck or Trailer Mounted Attenuator	DAY	26.000	26.000
0190	646.0106	Pavement Marking Epoxy 4-Inch	LF	157,774.000	157,774.000
0200	646.0126	Pavement Marking Epoxy 8-Inch	LF	5,018.000	5,018.000
0210	646.0600	Removing Pavement Markings	LF	162.000	162.000
0220	647.0166	Pavement Marking Arrows Epoxy Type 2	EACH	17.000	17.000
0230	647.0356	Pavement Marking Words Epoxy	EACH	17.000	17.000
0240	647.0566	Pavement Marking Stop Line Epoxy 18-Inch	LF	600.000	600.000
0250	648.0100	Locating No-Passing Zones	MI	7.500	7.500
0260	649.0403	Temporary Pavement Marking Epoxy 4-Inch	LF	37,831.250	37,831.250
0270	650.8000	Constructi on Staking Resurfacing Reference	LF	39,523.000	39,523.000
0280	690.0150	Sawing Asphalt	LF	34.000	34.000
0290	ASP.1TOA	On-the-Job Training Apprentice at \$5.00/HR	HRS	100.000	100.000
0300	ASP.1TOG	On-the-Job Training Graduate at \$5.00/HR	HRS	100.000	100.000
0310	SPV.0060	Special 01. Temporary Portable Rumble Strip Array	EACH	4.000	4.000

FINISHING ROADWAY (PROJECT)	
213. 0100	
LOCATION	EACH
ID. 2060- 17- 70	1
TOTAL 0010	
1	

BASE AGGREGATE DENSE					
3/4- INCH					
305. 0110					
STATION	TO	STATION	LOCATION	TON	REMARKS
35+40	-	35+80	STH 38 LT & RT	5	SEE PAVEMENT REPAIR DETAIL
TOTAL 0010				5	

ASPHALT CENTER LINE RUMBLE				
STRIPS 2-LANE RURAL				
465. 0475				
STATION	TO	STATION	LOCATION	LF
35+40	-	45+16	STH 38 R/L	976
49+20	-	65+40	STH 38 R/L	1620
69+40	-	80+60	STH 38 R/L	1120
82+77	-	99+56	STH 38 R/L	2943
101+84	-	112+20	STH 38 R/L	6957
116+19	-	171+41	STH 38 R/L	5522
175+41	-	199+15	STH 38 R/L	2374
201+14	-	212+60	STH 38 R/L	1146
234+00	-	239+72	STH 38 R/L	572
241+67	-	262+35	STH 38 R/L	2068
285+45	-	321+00	STH 38 R/L	3555
334+50	-	351+11	STH 38 R/L	1661
355+11	-	358+95	STH 38 R/L	384
364+95	-	367+80	STH 38 R/L	285
387+24	-	396+40	STH 38 R/L	916
400+58	-	403+87	STH 38 R/L	329
411+50	-	418+58	STH 38 R/L	708
422+58	-	426+16	STH 38 R/L	358
427+87	-	430+63	STH 38 R/L	276
TOTAL 0010				33770

ASPHALTIC SHOULDER RUMBLE				
STRIPS 2-LANE RURAL				
465. 0425				
STATION	TO	STATION	LOCATION	LF
BEGIN PROJECT	-	HOOD' S CREEK RD	STH 38 RT.	741
HOOD' S CREEK RD	-	FOUR MILE RD	STH 38 RT.	4610
FOUR MILE RD	-	FIVE MILE RD	STH 38 RT.	4088
FIVE MILE RD	-	CRISTY CIRCLE DR.	STH 38 RT.	3268
SIX MILE RD	-	HUSHER (UNINCORP.)	STH 38 RT.	2671
HUSHER (UNINCORP.)	-	CTH H	STH 38 RT.	3351
CTH H	-	SEVEN MILE RD.	STH 38 RT.	1657
SEVEN MILE RD.	-	END PROJET	STH 38 RT.	2687
BEGIN PROJECT	-	HOOD' S CREEK RD	STH 38 LT.	958
HOOD' S CREEK RD	-	FOUR MILE RD	STH 38 LT.	4817
FOUR MILE RD	-	FIVE MILE RD	STH 38 LT.	4800
FIVE MILE RD	-	LINWOOD RD	STH 38 LT.	3051
SIX MILE RD	-	HUSHER (UNINCORP.)	STH 38 LT.	2578
HUSHER (UNINCORP.)	-	CTH H	STH 38 LT.	3403
CTH H	-	SEVEN MILE RD.	STH 38 LT.	3151
SEVEN MILE RD.	-	END PROJET	STH 38 LT.	2853
TOTAL 0010				48684

MOBILIZATION 619. 1000	
LOCATION	EACH
ID 2060- 17- 70	1
TOTAL 0010	1

FIELD OFFICE TYPE B 642. 5001	
LOCATION	EACH
ID 2060- 17- 70	1
TOTAL 0010	1

TRAFFIC CONTROL (PROJECT) 643. 0100	
LOCATION	EACH
ID 2060- 17- 70	1
TOTAL 0010	1

TEMPORARY PORTABLE RUMBLE STRIP ARRAY SPV. 0060. 01		
STATION	LOCATION	EACH
PROJECT LIMITS	STH 38	4
TOTAL 0010		4

REMOVING PAVEMENT MARKINGS 646. 0600					
STATION	TO	STATION	LOCATION	LF	REMARKS
62+00	-	65+65	12' LT.	92	3' SKIPS
68+80	-	71+60	12' LT.	70	3' SKIPS
TOTAL 0010				162	

TEMPORARY PAVEMENT MARKING EPOXY 4-INCH (YELLOW) 649. 0403					
STATION	TO	STATION	LOCATION	LF	REMARKS
35+40	-	45+16	STH 38 R/L	1952	LONGITUDINAL JOINT REPAIR
49+20	-	65+40	STH 38 R/L	3240	LONGITUDINAL JOINT REPAIR
69+40	-	80+60	STH 38 R/L	2240	LONGITUDINAL JOINT REPAIR
82+77	-	99+56	STH 38 R/L	3358	LONGITUDINAL JOINT REPAIR
101+84	-	112+20	STH 38 R/L	2072	LONGITUDINAL JOINT REPAIR
116+19	-	171+41	STH 38 R/L	11044	LONGITUDINAL JOINT REPAIR
175+41	-	199+15	STH 38 R/L	3266	LONGITUDINAL JOINT REPAIR
201+14	-	212+60	STH 38 R/L	1760	LONGITUDINAL JOINT REPAIR
234+00	-	239+72	STH 38 R/L	1144	LONGITUDINAL JOINT REPAIR
241+67	-	262+35	STH 38 R/L	2735	LONGITUDINAL JOINT REPAIR
345+50	-	347+00	STH 38 R/L	300	LONGITUDINAL JOINT REPAIR
387+24	-	396+40	STH 38 R/L	1378	LONGITUDINAL JOINT REPAIR
400+58	-	403+87	STH 38 R/L	658	LONGITUDINAL JOINT REPAIR
411+50	-	418+58	STH 38 R/L	1416	LONGITUDINAL JOINT REPAIR
422+58	-	426+16	STH 38 R/L	716	LONGITUDINAL JOINT REPAIR
427+87	-	430+63	STH 38 R/L	552	LONGITUDINAL JOINT REPAIR
TOTAL 0010				37831	

NOTE: APPLY TEMPORARY PAVEMENT MARKING TO LONGITUDINAL REPAIR AREAS AS DETERMINED BY "LOCATING NO PASSING ZONES".

CONSTRUCTION STAKING RESURFACING REFERENCE				
STATION	TO	STATION	LOCATION	650. 8000 LF
35+40	-	430+63	STH 38 R/L	39523
TOTAL 0010				39523

				REMOVING ASPHALTIC SURFACE MILLING 204. 0125	TACK COAT 455. 0605	INCENTIVE DENSITY HMA PAVEMENT 460. 2000	HMA PAVEMENT 4 MI 58- 28 S 460. 6224	
STATION	TO	STATION	LOCATION	TON	GAL	DOL	TON	REMARKS
35+40	-	45+16	STH 38	33	19	22	33	BEGIN PROJECT
49+20	-	65+40	STH 38	54	32	35	54	
69+40	-	80+60	STH 38	38	22	25	38	
82+77	-	99+56	STH 38	56	33	36	56	
101+84	-	112+20	STH 38	35	21	23	35	
116+19	-	171+41	STH 38	185	108	119	185	
175+41	-	199+15	STH 38	80	47	52	80	
201+14	-	212+60	STH 38	39	23	25	39	
234+00	-	239+72	STH 38	20	12	13	20	
241+67	-	262+35	STH 38	69	41	45	69	
345+50	-	347+00	STH 38	5	3	4	5	
387+24	-	396+40	STH 38	31	18	20	31	
400+58	-	403+87	STH 38	11	7	8	11	
411+50	-	418+58	STH 38	24	14	16	24	
422+58	-	426+16	STH 38	12	7	8	12	
427+87	-	430+63	STH 38	10	6	7	10	END PROJECT NORTH COUNTY LINE
35+40	-	35+80	STH 38	50	1	32	50	SEE PAVEMENT REPAIR DETAIL
		UNDI STRI BUTED	STH 38	25	15	16	25	
TOTAL 0010				777	429	510	777	

STATION	TO	STATION	LOCATION	MOVING SIGNS	LOCATING NO-PASSING
				TYPE II 638. 2102 EACH	ZONES 648. 0100 MI
35+40	-	430+63	STH 38 R/L	8	7. 5
TOTAL 0010				8	7. 5

STATION	TO	STATION	LOCATION	POSTS WOOD 4X6- INCH X 16- FT 634. 0616 EACH
35+40	-	430+63	STH 38 R/L	8
TOTAL 0010				8

STATION	TO	STATION	LOCATION	REMOVING SMALL SIGN SUPPORTS 638. 3000 EACH
35+40	-	430+63	STH 38 R/L	8
TOTAL 0010				8

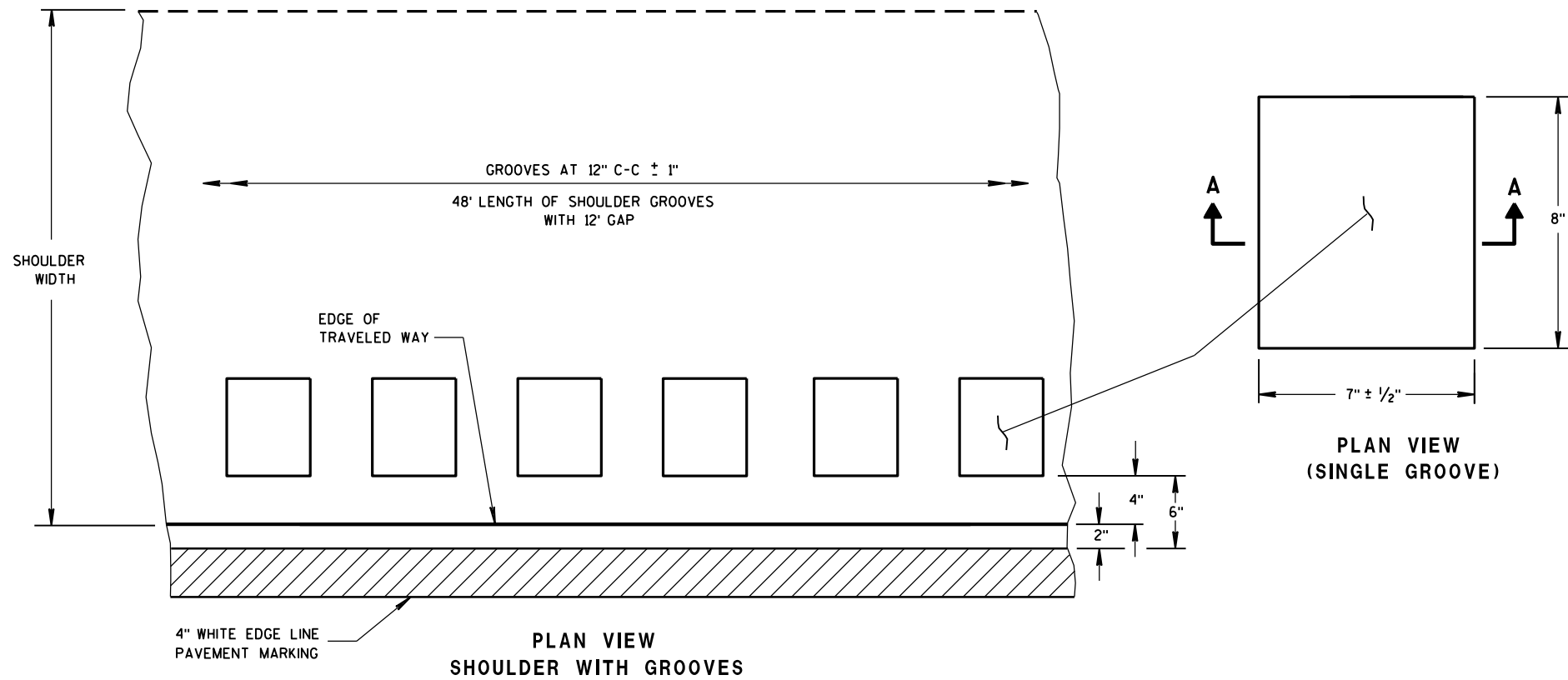
STATION	TO	STATION	LOCATION	PAVEMENT MARKING	PAVEMENT MARKING	PAVEMENT MARKING	PAVEMENT MARKING	PAVEMENT MARKING
				EPOXY 4-INCH	EPOXY 8-INCH	ARROWS EPOXY	MARKING	STOP LINE EPOXY
				646. 0106	646. 0126	TYPE 2	WORDS EPOXY	18-INCH
				LF	LF	EACH	EACH	LF
35+40	-	220+00	STH 38	70665	--	--	--	--
		INTERSECTIONS	HOOD' S CREEK RD.	--	200	--	--	22
			BROOK ROAD	--	235	--	--	--
			FOUR MILE ROAD	608	327	--	--	80
			FIVE MILE ROAD	690	400	--	--	62
220+00	-	430+63	STH 38	84635	--	--	--	--
		INTERSECTIONS	LINWOOD ROAD	--	--	--	--	18
			SIX MILE ROAD	710	1653	5	5	154
			NICHOLSON ROAD	--	--	--	--	44
			CTH "H"	--	482	4	4	34
			FOREST ROAD	--	100	--	--	20
			SEVEN MILE ROAD	466	1366	8	8	116
			SEVEN & HALF MILE ROAD	--	--	--	--	24
			CADDY LANE	--	255	--	--	26
TOTAL 0010				157774	5018	17	17	600

	TRAFFIC CONTROL DRUMS 643. 0300	TRAFFIC CONTROL SIGNS 643. 0900	TRAFFIC CONTROL SIGNS PCMS 643. 1050	TRUCK OR TRAILER MOUNTED ATTENUATOR 643. 1055. S
LOCATION	DAY	DAY	DAY	DAY
STH 38	300	1320	42	26
UNDISTRI BUTED	150	80	--	--
TOTAL 0010	450	1400	42	26

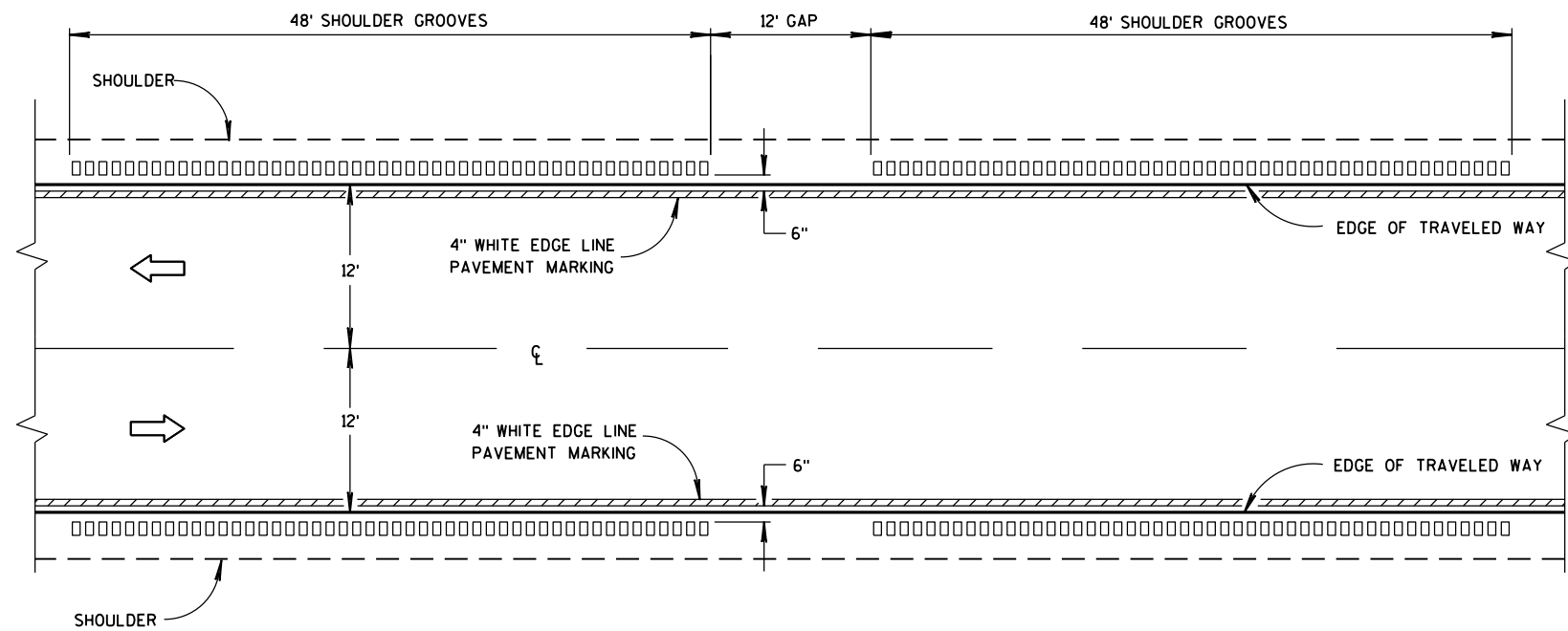
		SAWING ASPHALT 690. 0150	
STATION	LOCATION	LF	REMARKS
35+80	STH 38 R/L	34	SEE PAVEMENT REPAIR DETAIL
	TOTAL 0010	34	

Standard Detail Drawing List

13A10-01A	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-01C	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A11-02A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-02B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
15C04-02	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C07-12B	PAVEMENT MARKING WORDS
15C07-12C	PAVEMENT MARKING ARROWS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C08-16C	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16D	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16E	PAVEMENT MARKING (LEFT TURN LANE)
15C19-03A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C28-02	SIGNING AND MARKING FOR COMBINATION RIGHT TURN AND BYPASS LANE
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING



6
PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP



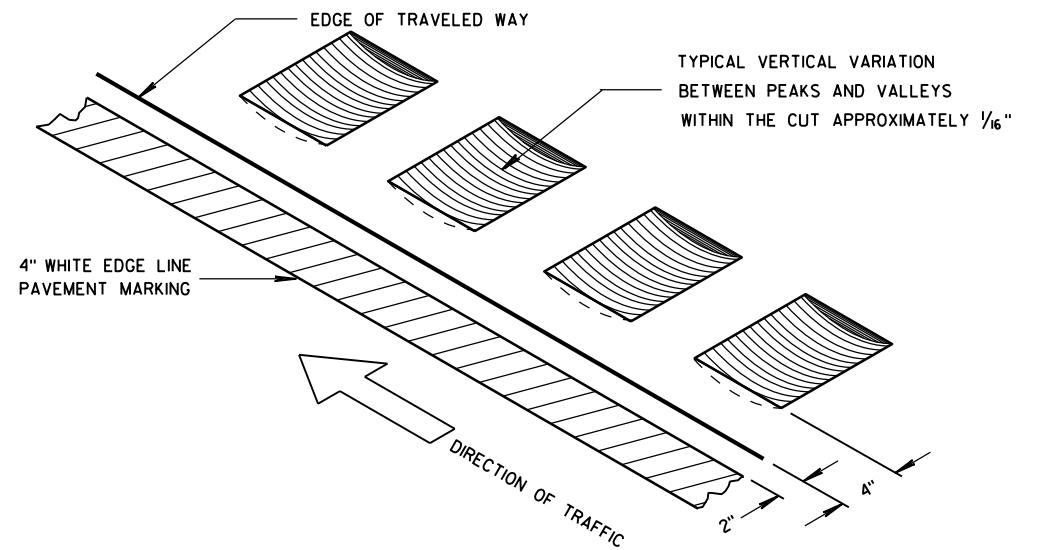
TYPE 1
2-LANE SHOULDER RUMBLE STRIP

GENERAL NOTES

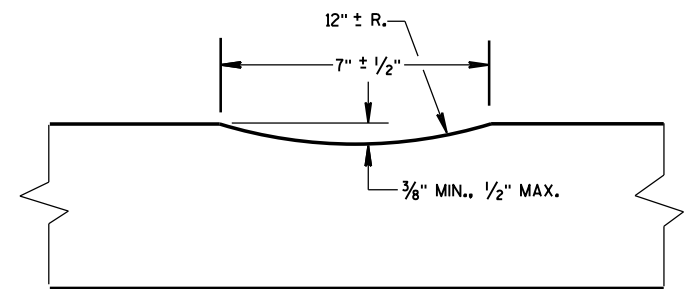
DETAILS OF CONSTRUCTION SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



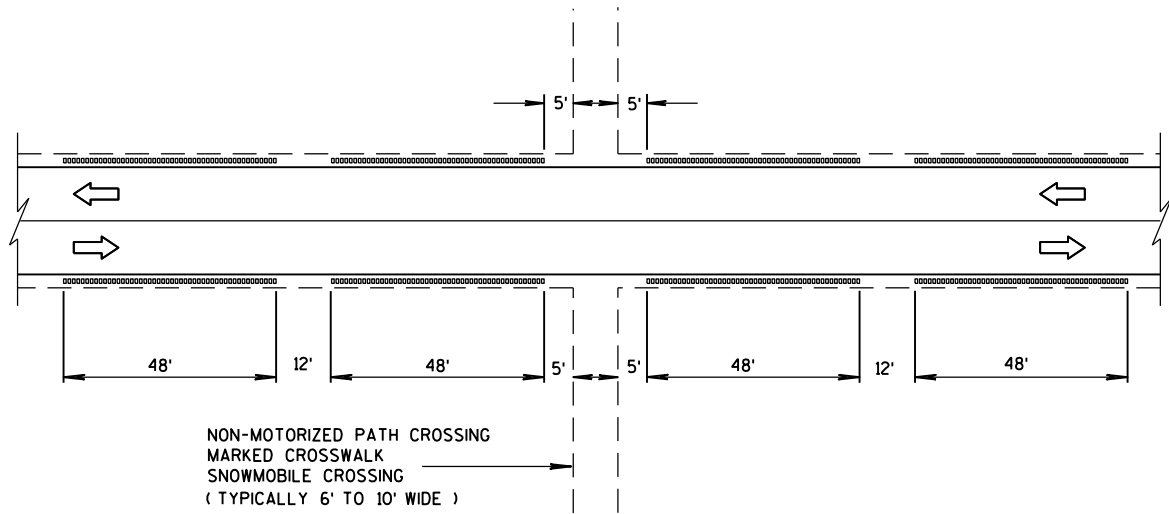
ISOMETRIC



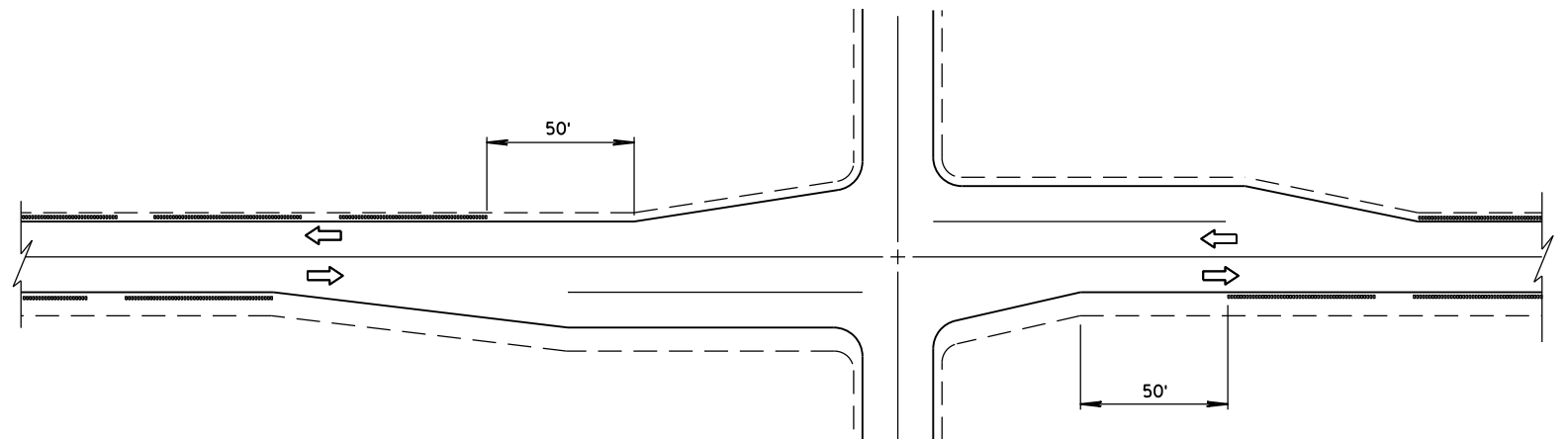
SECTION A-A

2-LANE RURAL
SHOULDER RUMBLE STRIP, MILLING

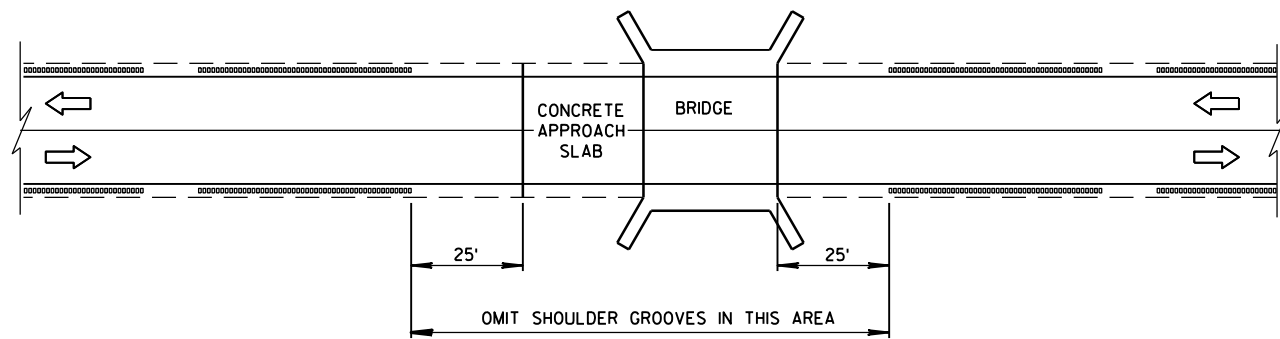
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



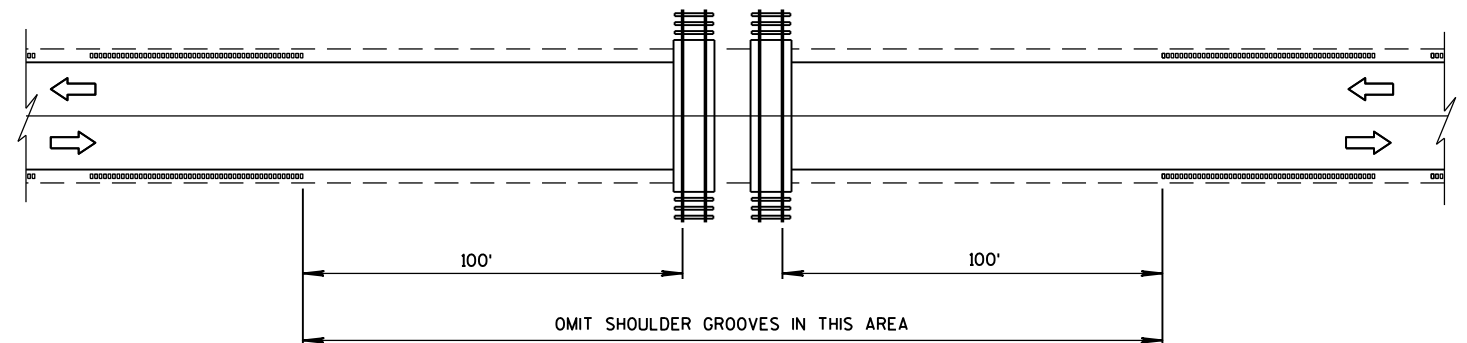
SHOULDER GROOVES AT MISCELLANEOUS CROSSINGS



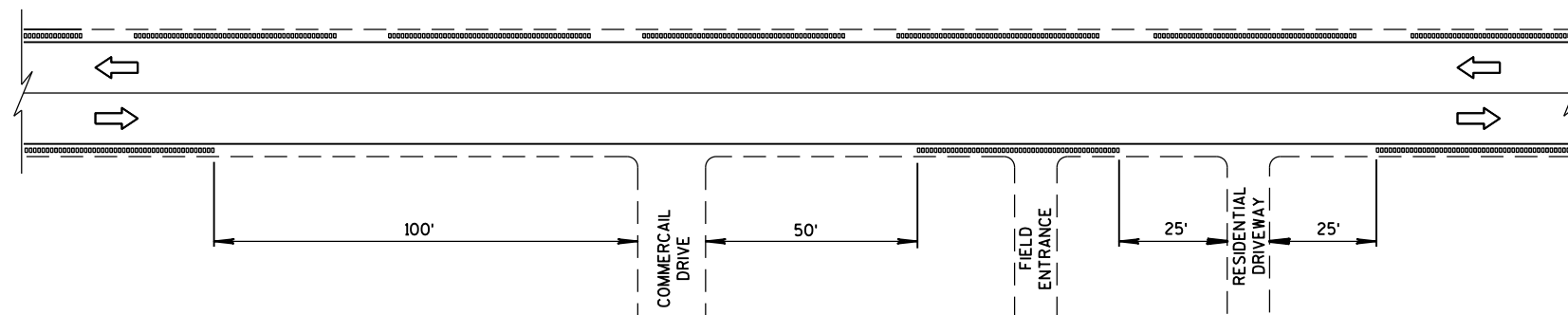
SHOULDER GROOVES AT INTERSECTIONS



SHOULDER GROOVES AT BRIDGES



SHOULDER GROOVES AT RAILROADS



SHOULDER GROOVES AT DRIVEWAYS^①

2-LANE RURAL
SHOULDER RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

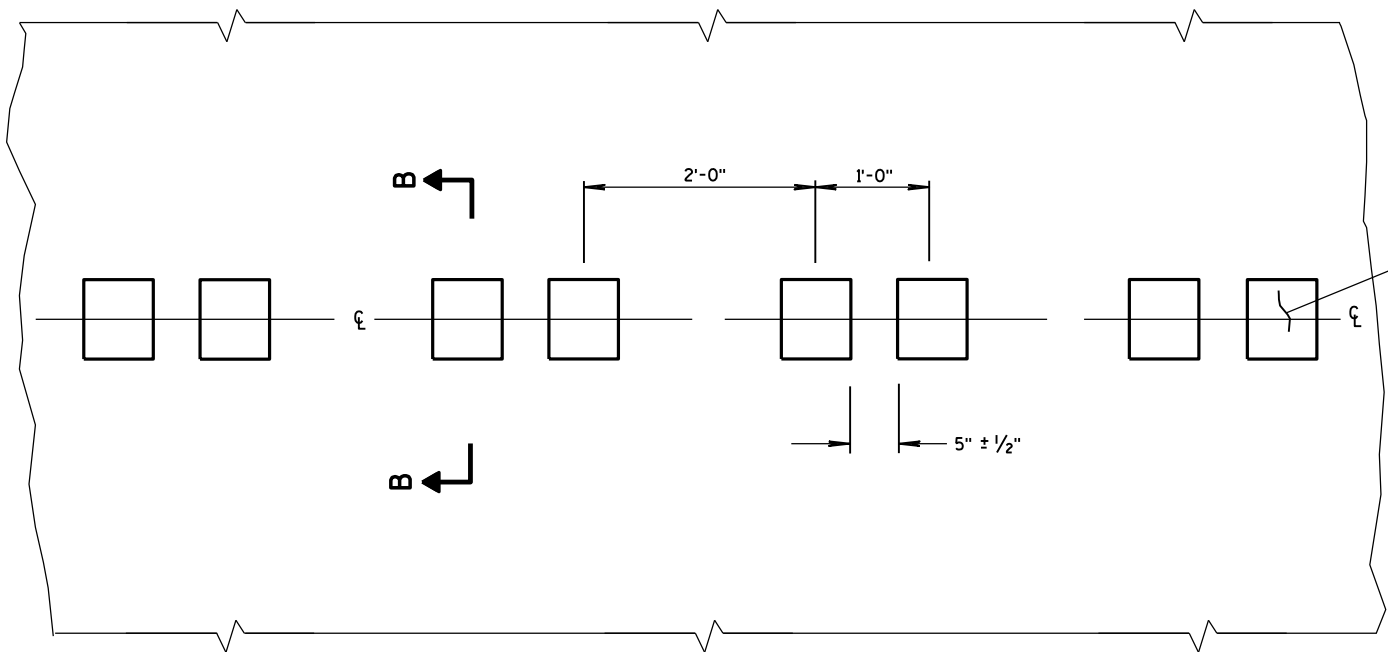
DETAILS OF CONSTRUCTION SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTER LINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

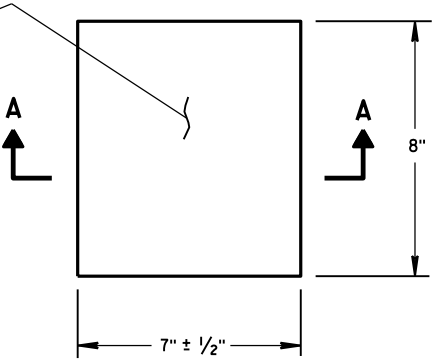
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

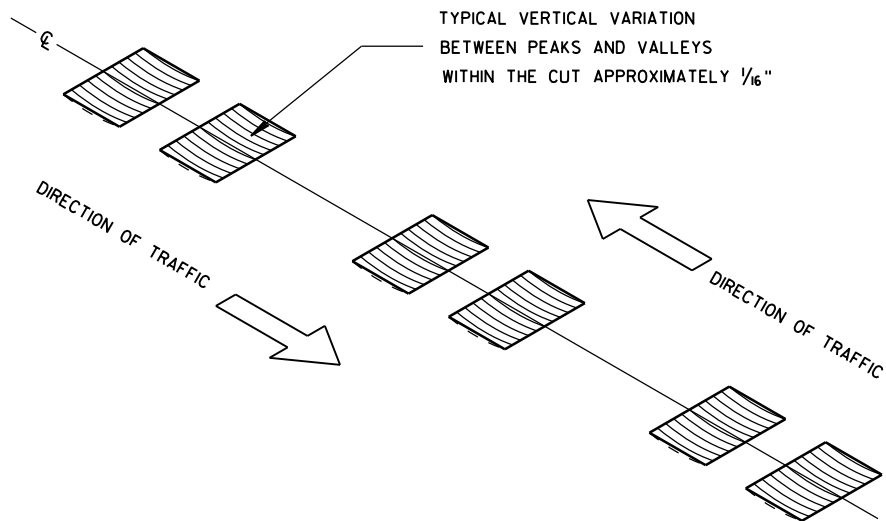
- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



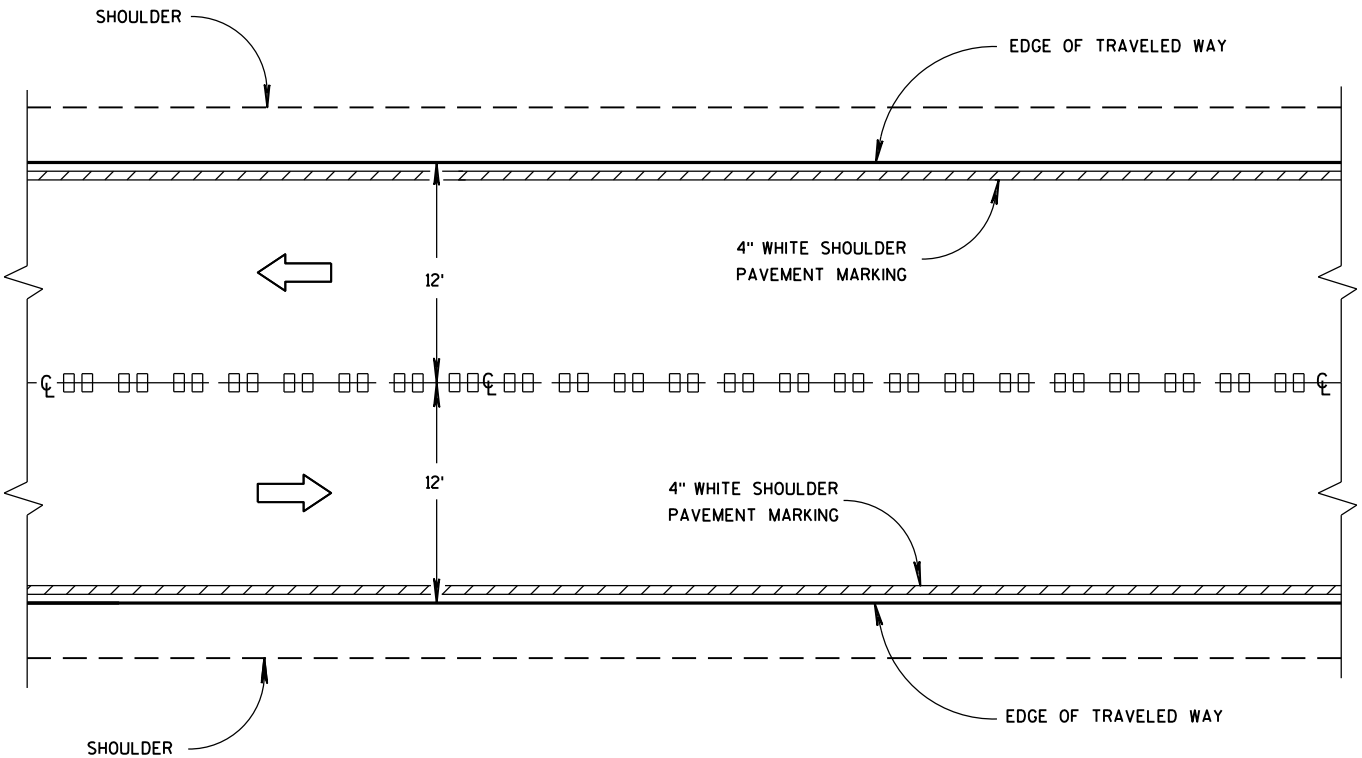
PLAN VIEW
CENTER LINE WITH GROOVES



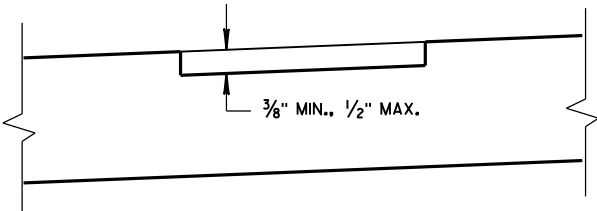
PLAN VIEW
(SINGLE GROOVE)



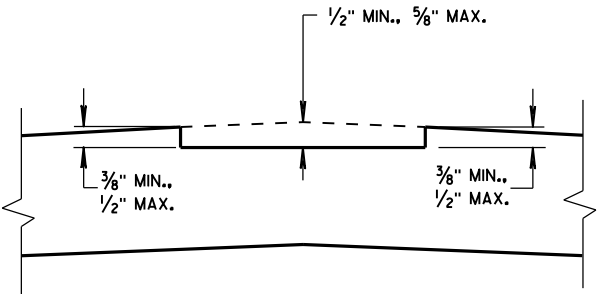
ISOMETRIC



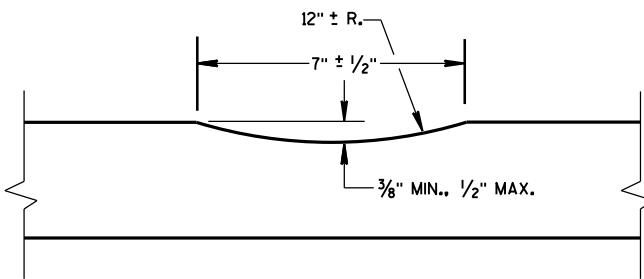
CENTER LINE GROOVES ON TWO-WAY ROADWAYS



SECTION B-B
SUPERELEVATED ROADWAY



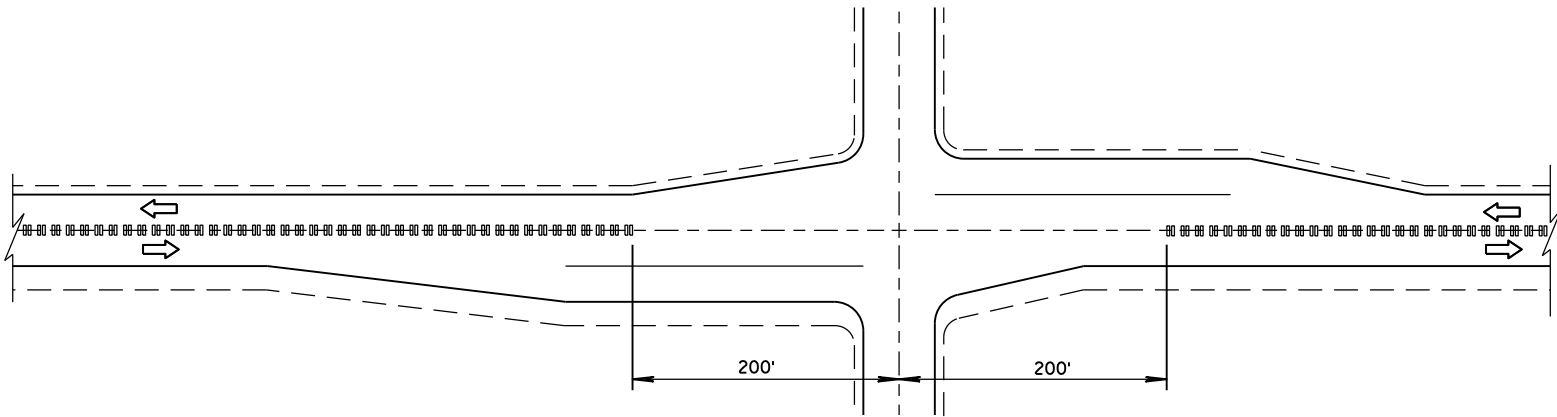
SECTION B-B
CROWNED ROADWAY



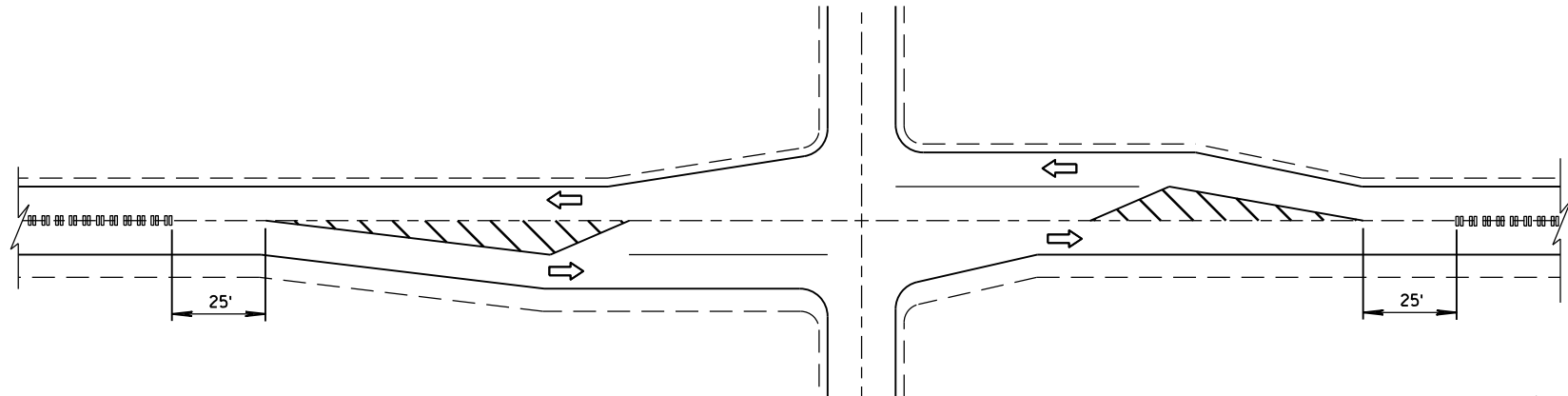
SECTION A-A

2-LANE RURAL
CENTER LINE RUMBLE STRIP,
MILLING

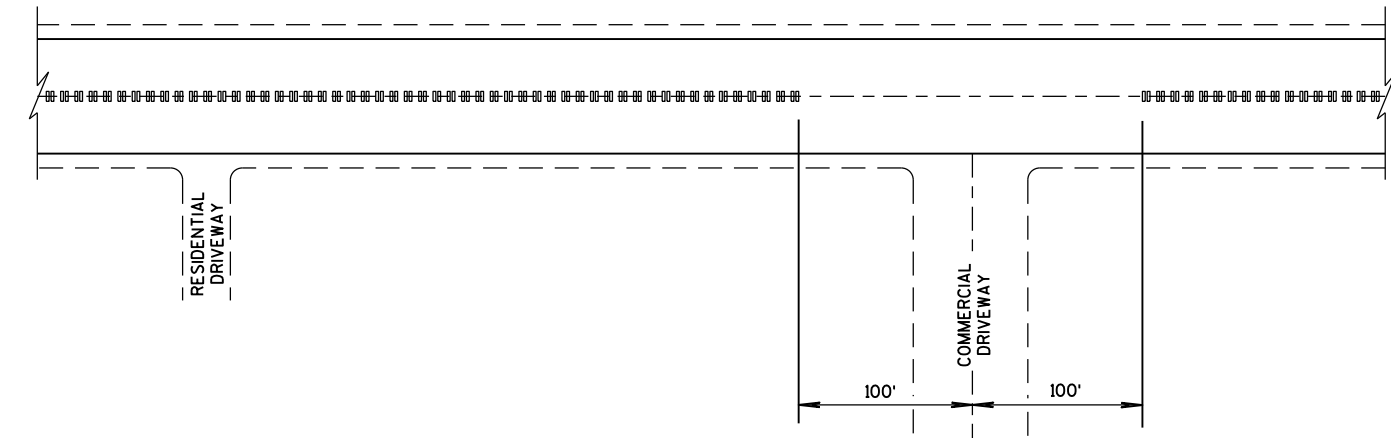
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CENTER LINE GROOVES AT INTERSECTIONS

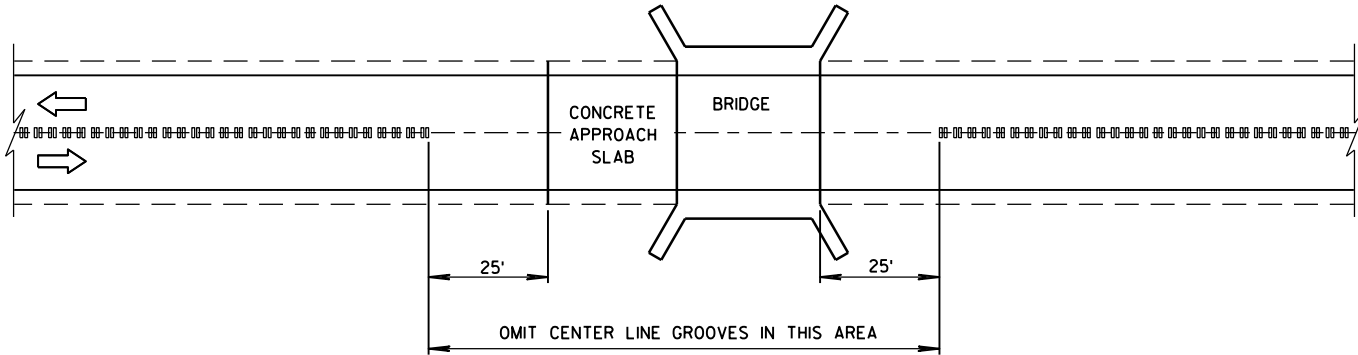


CENTER LINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)

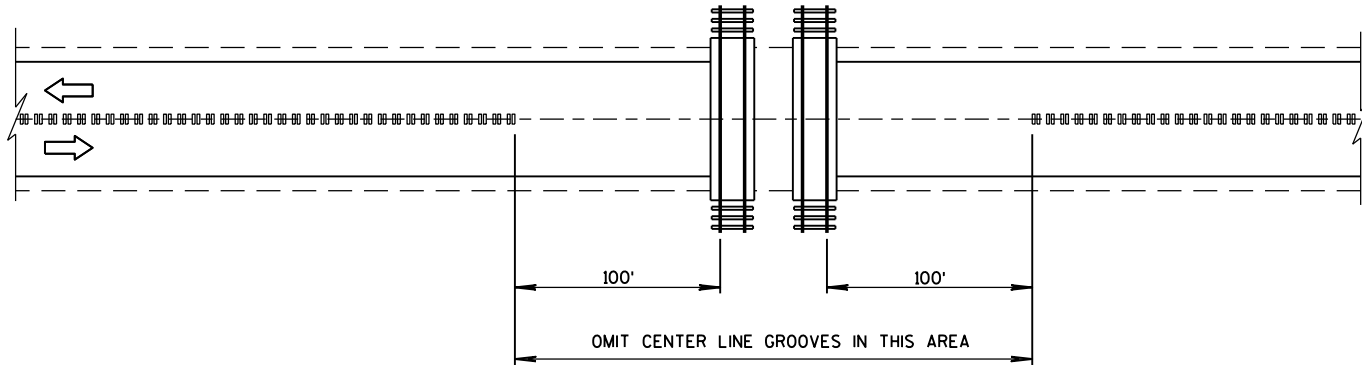


CENTER LINE GROOVES AT DRIVEWAYS^①

① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.

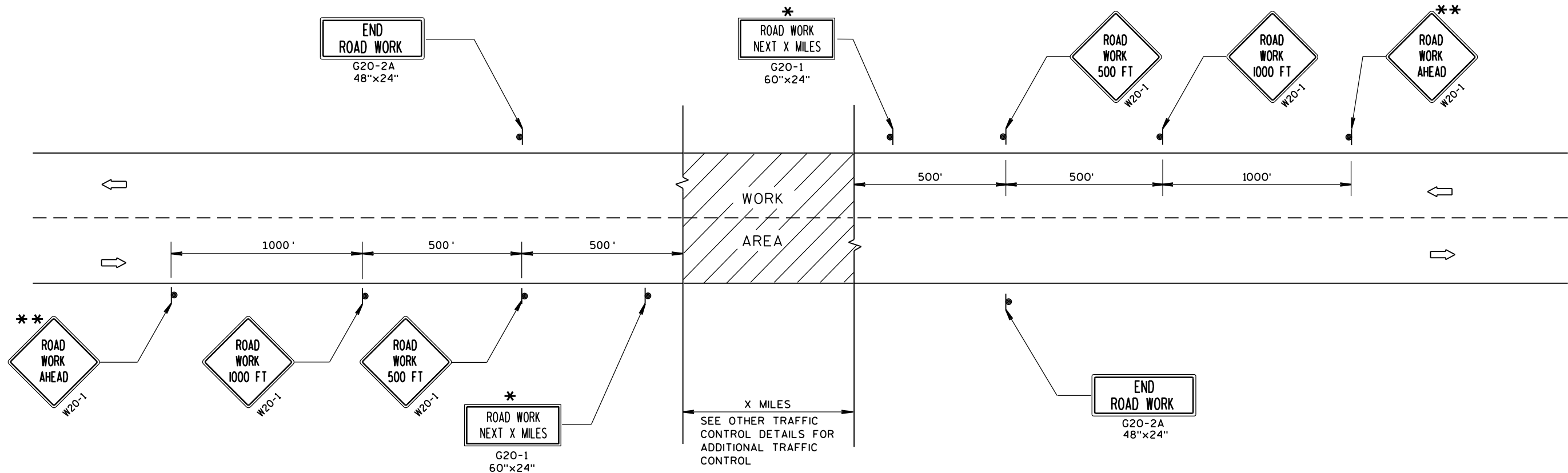


CENTER LINE GROOVES AT BRIDGES



CENTER LINE GROOVES AT RAILROADS

2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 5/15/2013 DATE	/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

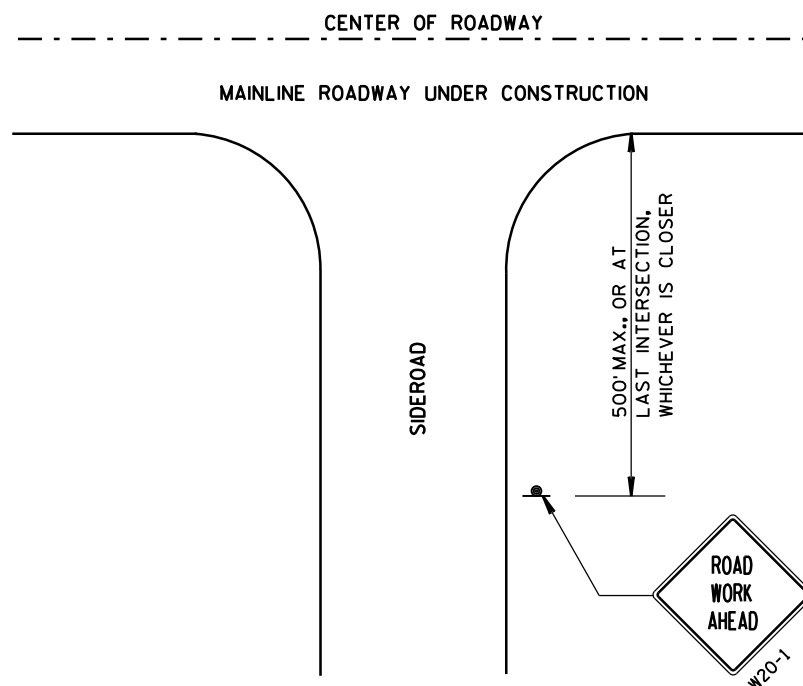
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

** PLACE ADDITIONAL W20-1 "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 45 M.P.H.
OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

8/2013

DATE

FHWA

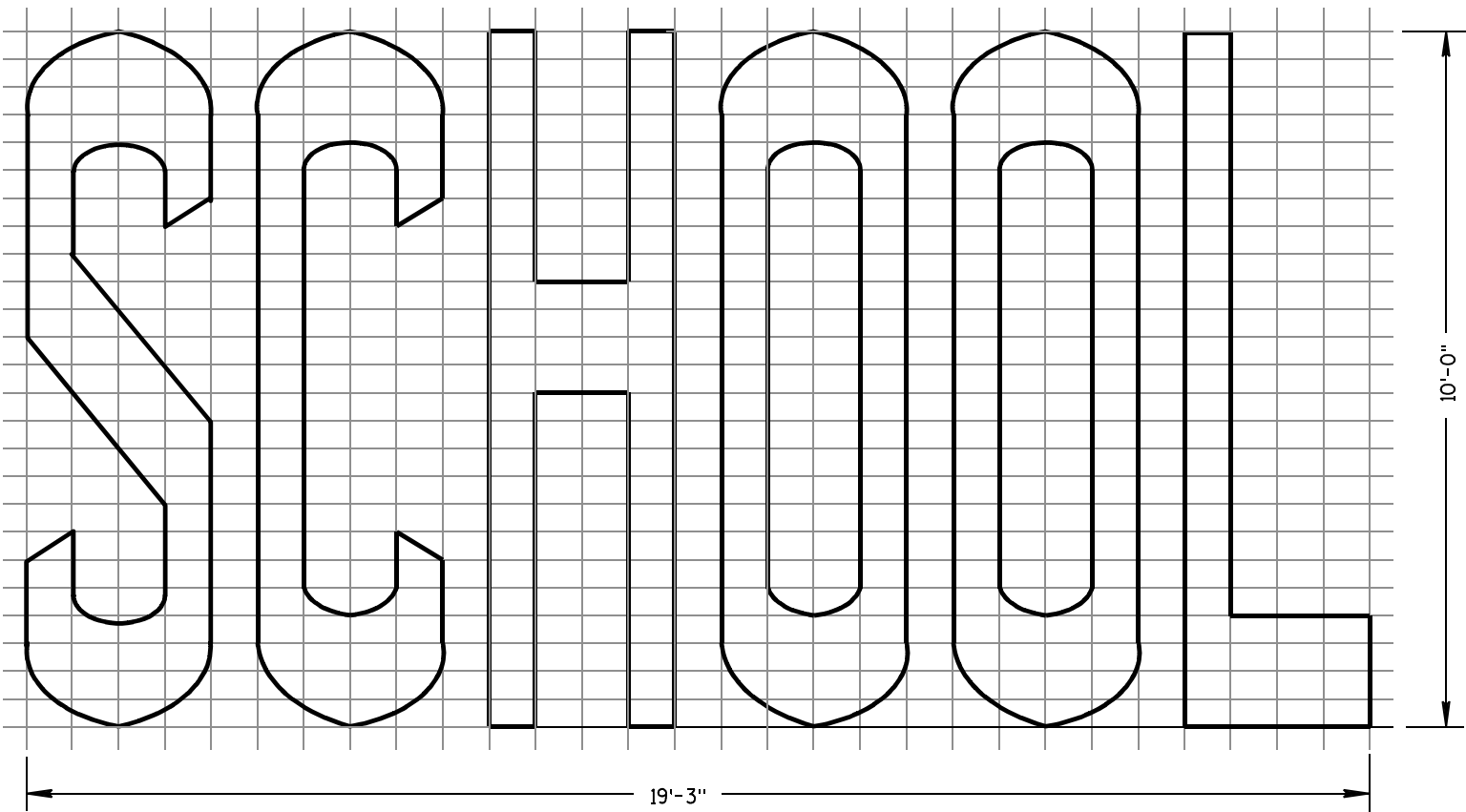
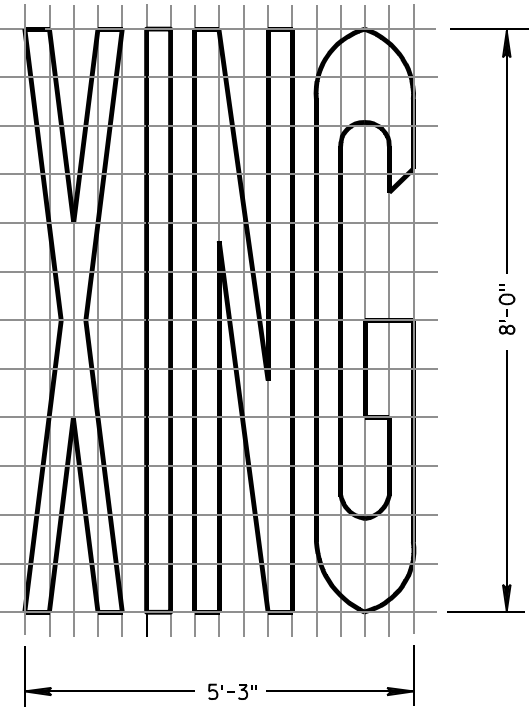
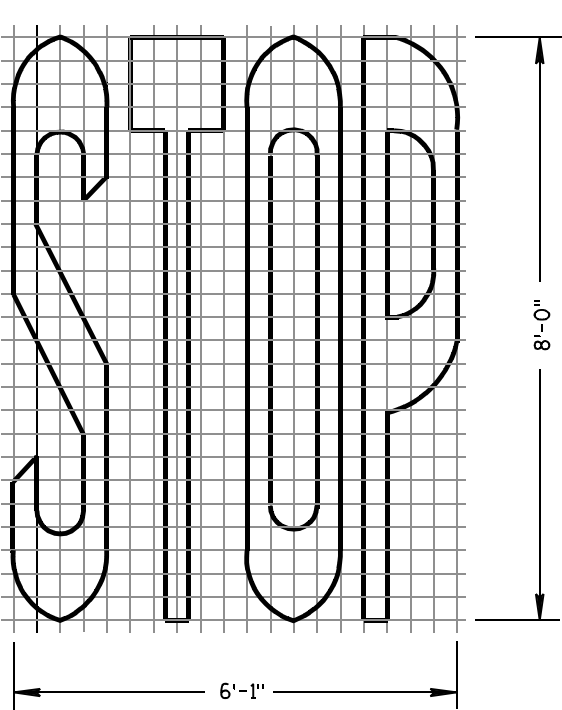
/S/ Travis Feltes

STATE TRAFFIC ENGINEER OF DESIGN

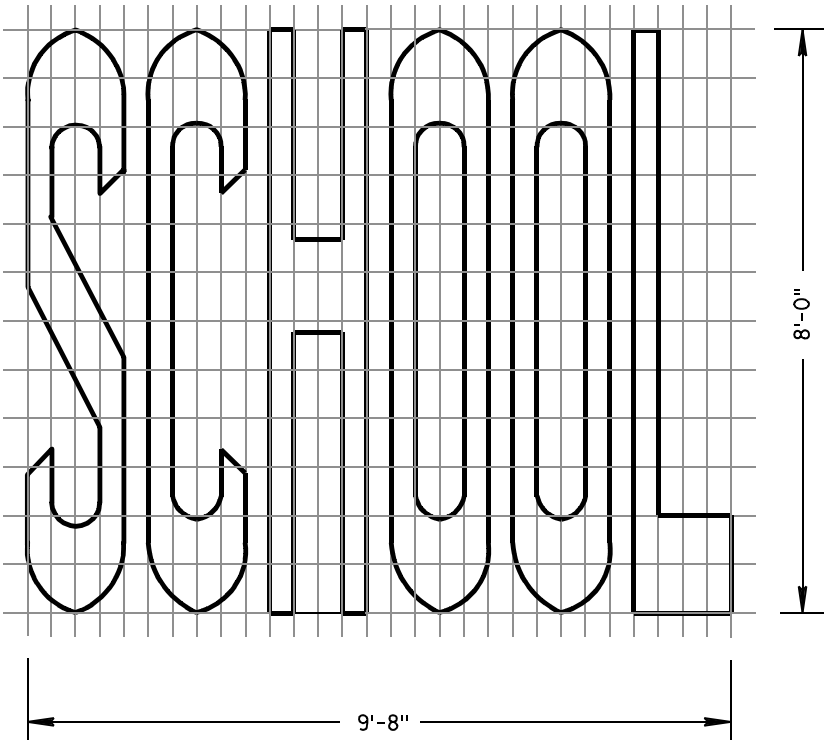
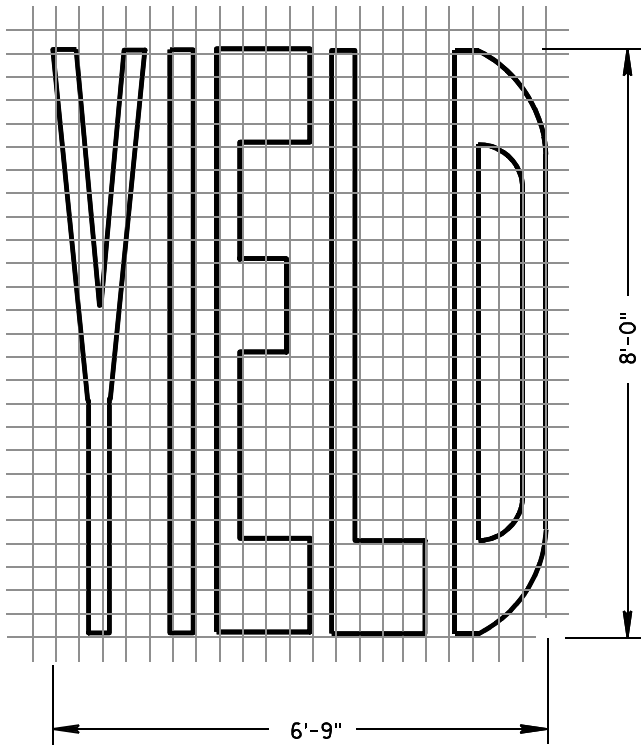
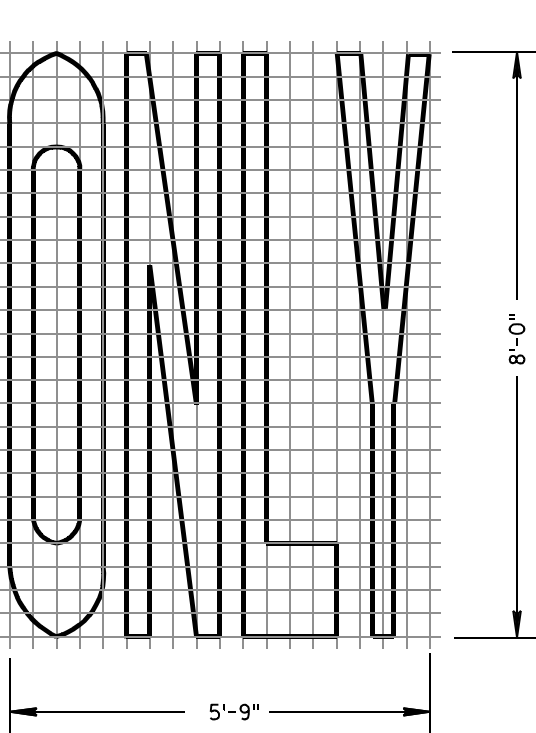
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

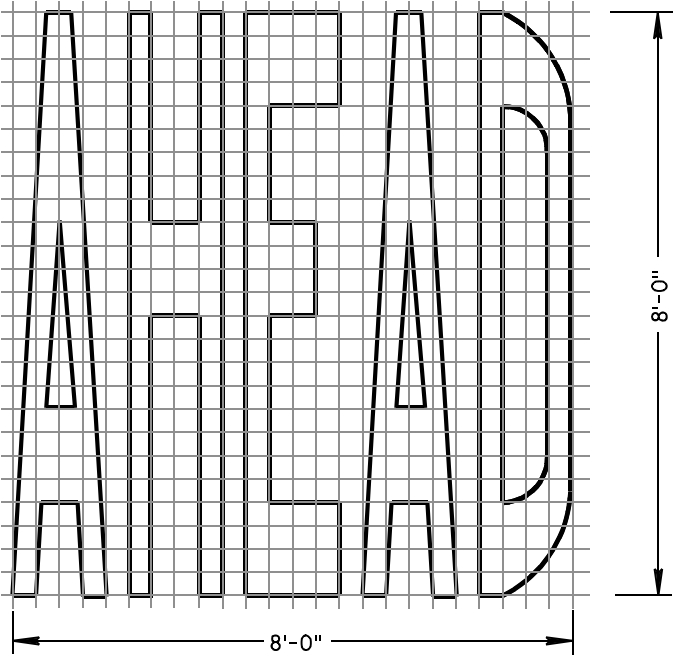
ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



TWO-LANE



SINGLE-LANE



PAVEMENT MARKING WORDS

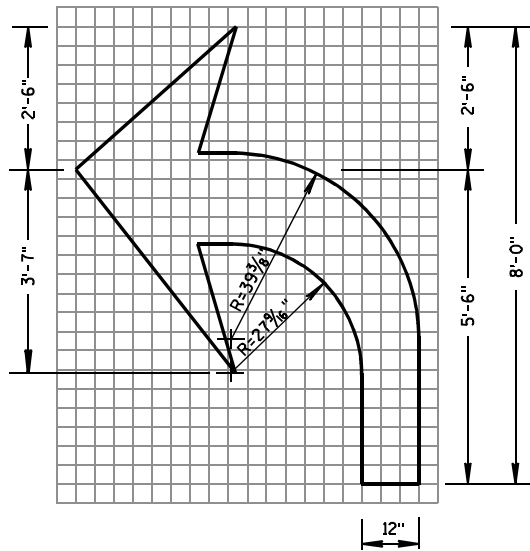
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

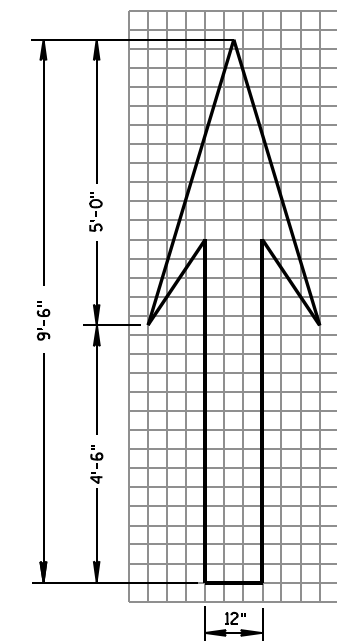
7-1-11
DATE

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

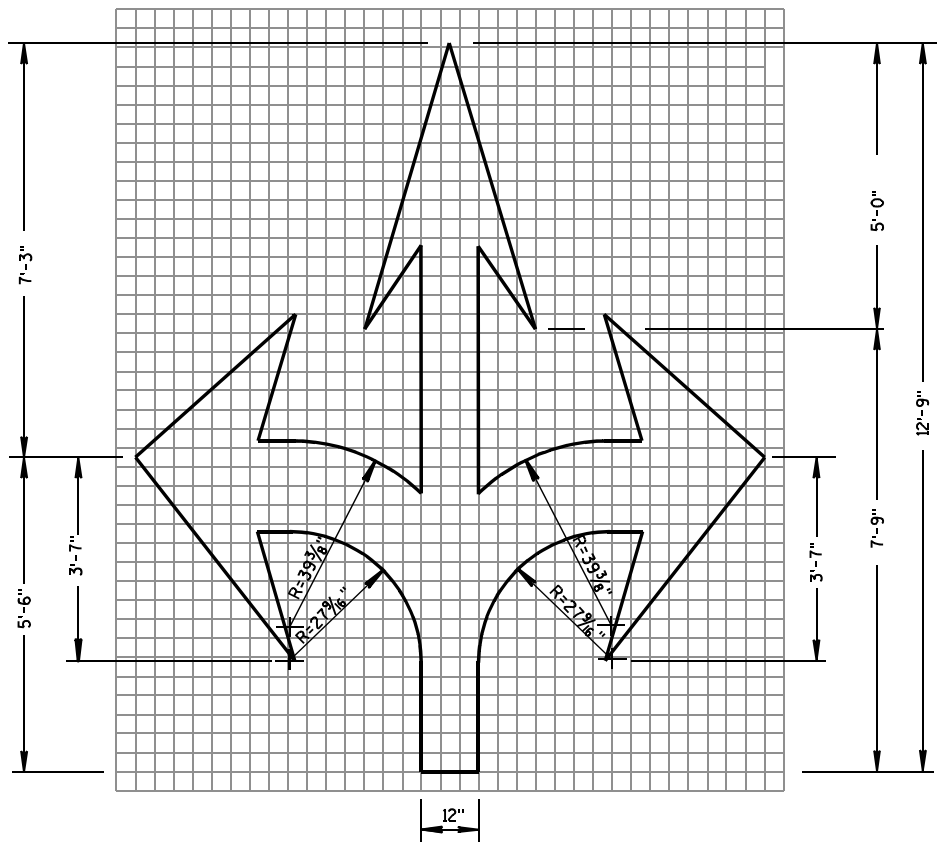
FHWA



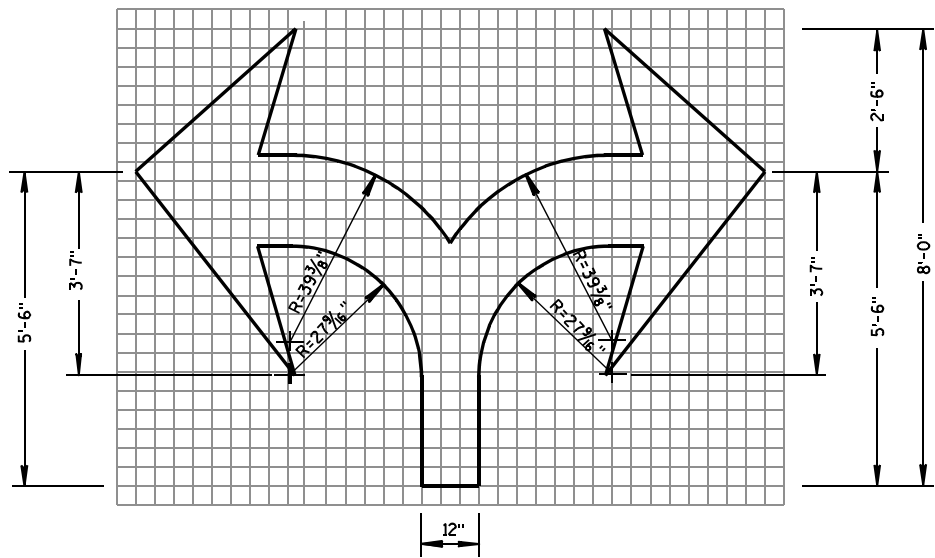
TYPE 2



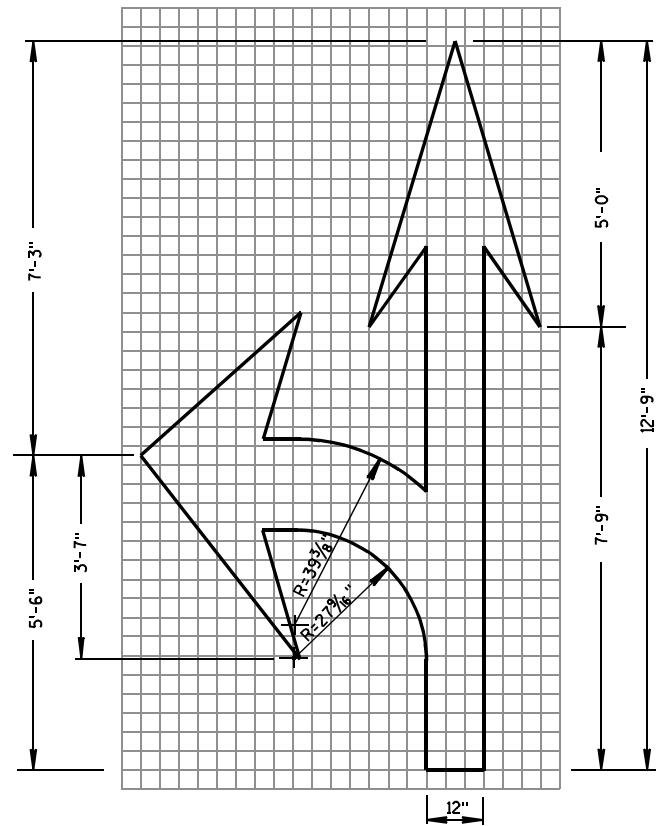
TYPE 1



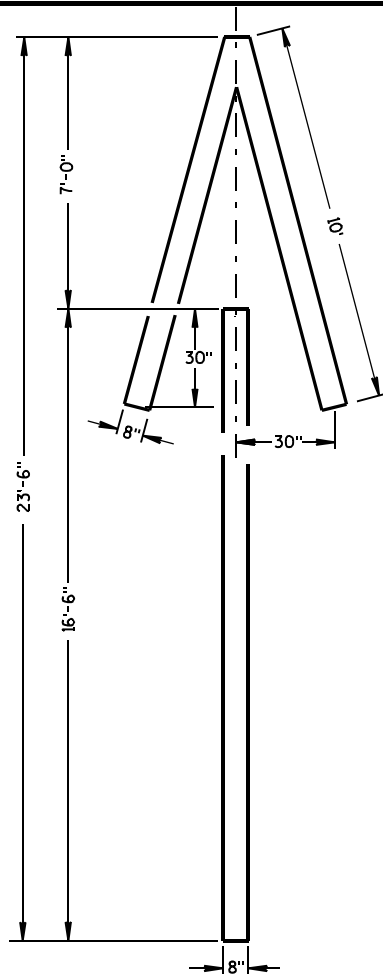
TYPE 6



TYPE 7



TYPE 3

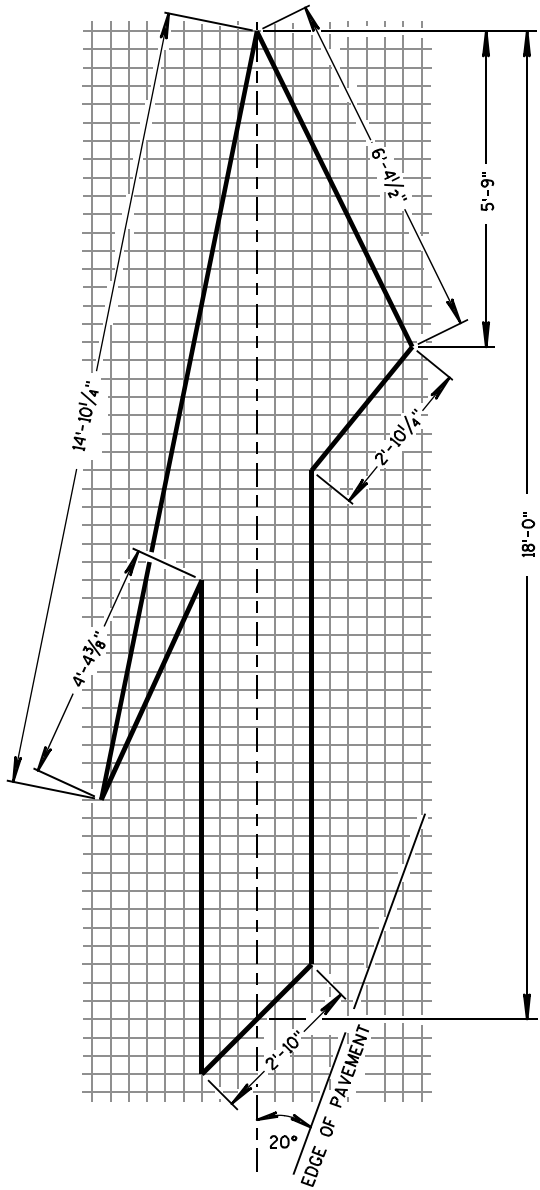


TYPE 4

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

ALL LETTERS, ARROWS AND SYMBOLS SHALL BE IN CONFORMANCE WITH REQUIREMENTS INCLUDED IN "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKING" BOOK BY THE FEDERAL HIGHWAY ADMINISTRATION. ALL LETTERS, ARROWS AND SYMBOLS SHALL BE WHITE AND REFLECTORIZED. SMALL DIFFERENCES IN DIMENSIONS WITHIN THE TOLERANCES OF THAT BOOK ARE ACCEPTABLE.



TYPE 5 LANE DROP ARROW

PAVEMENT MARKING ARROWS

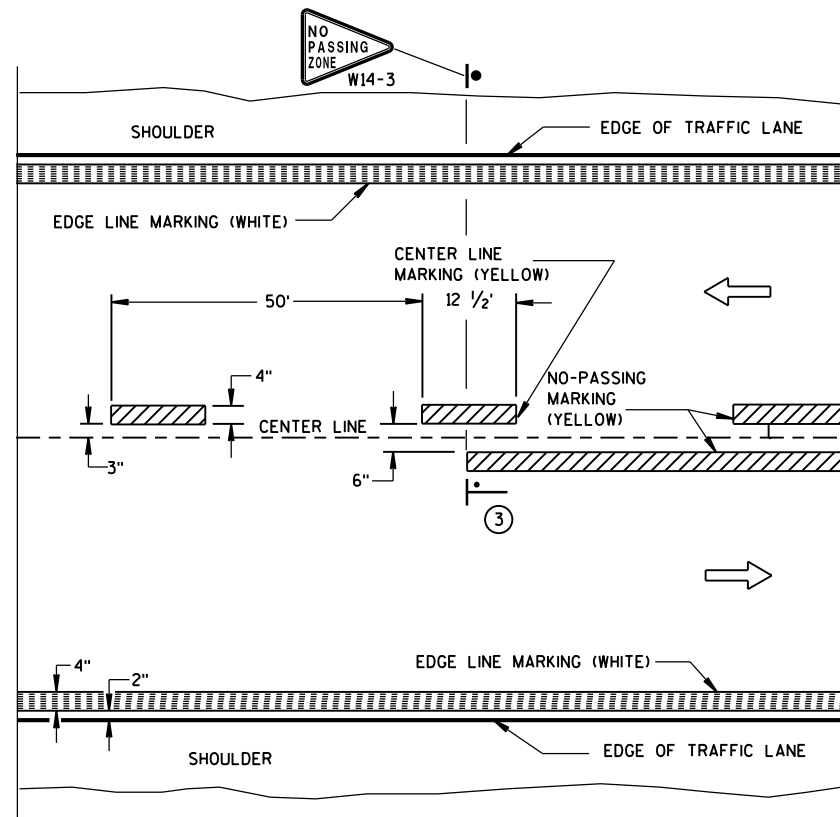
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

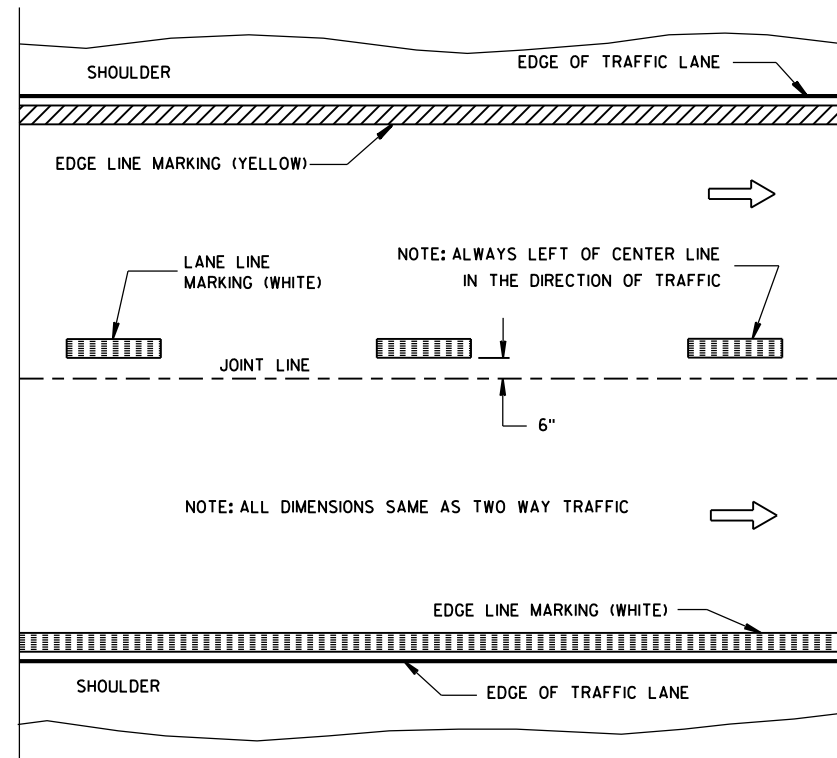
7/1/11
DATE

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

FHWA

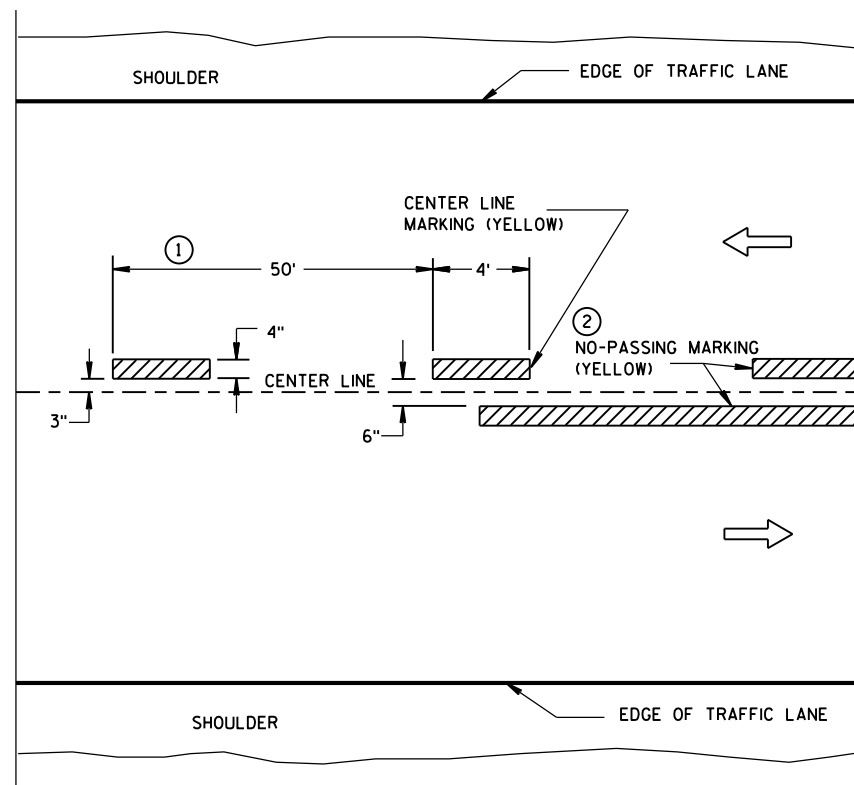


TWO WAY TRAFFIC

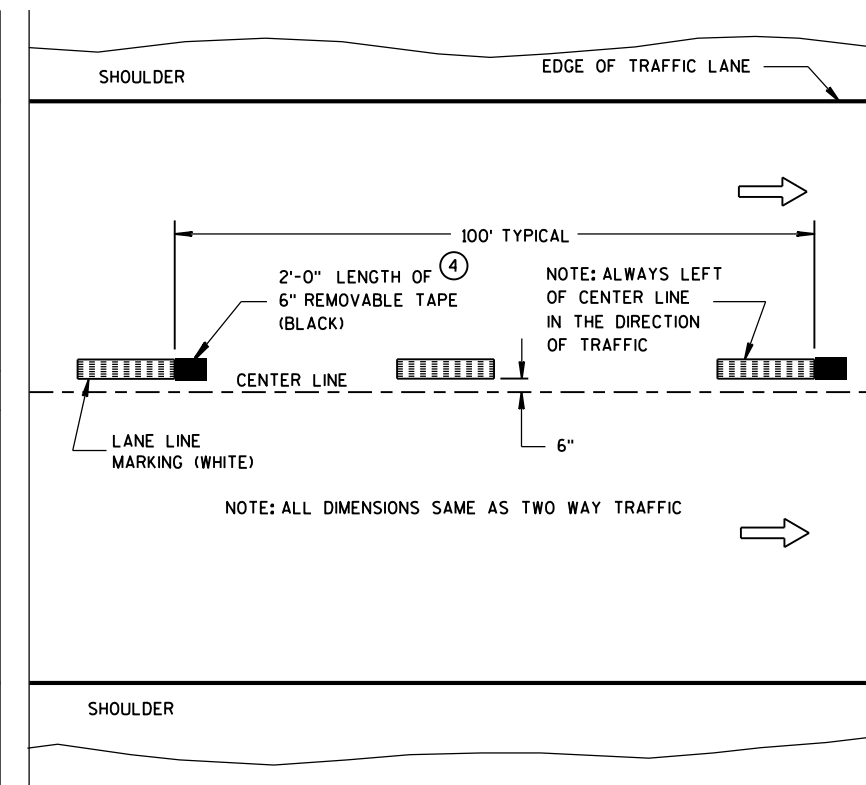


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

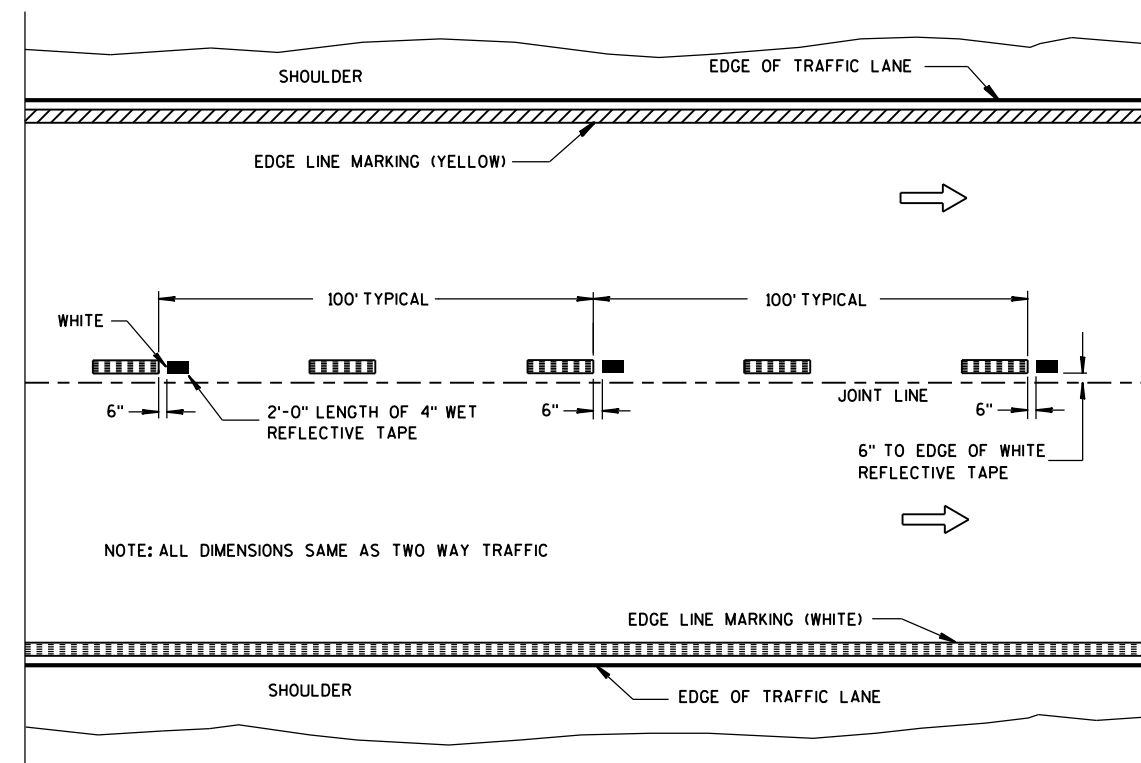
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- ② NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- ③ NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- ④ CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

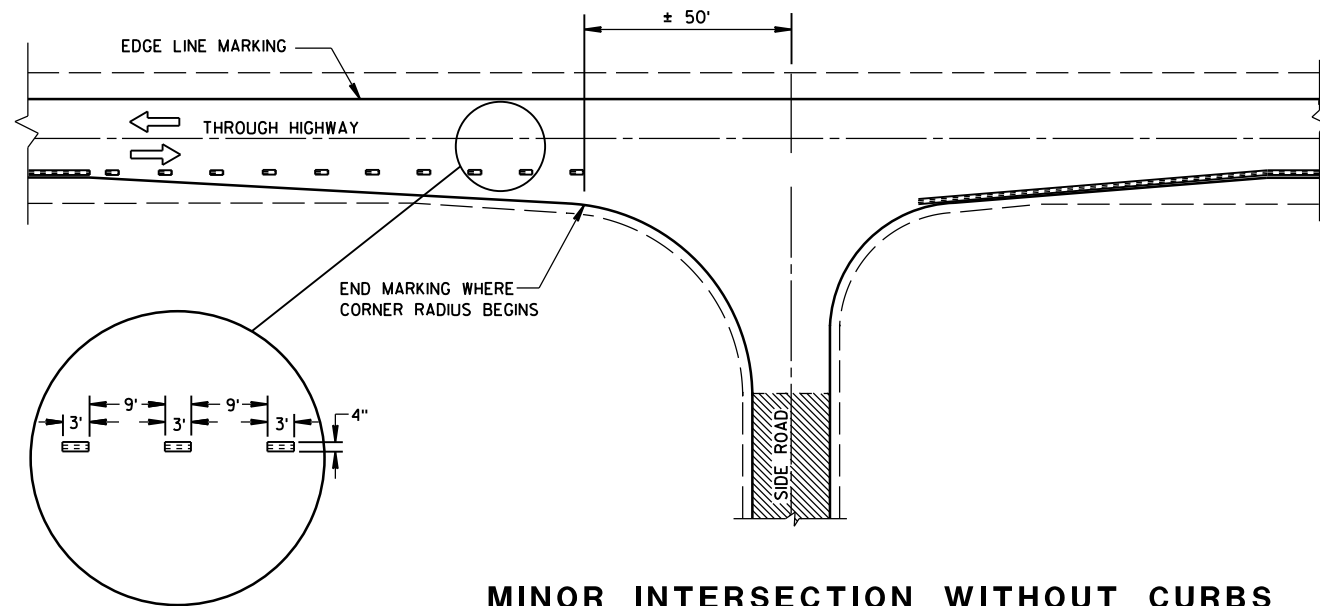
- "T" MARKING
- POST MOUNTED SIGN

PAVEMENT MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013
DATE
FHWA

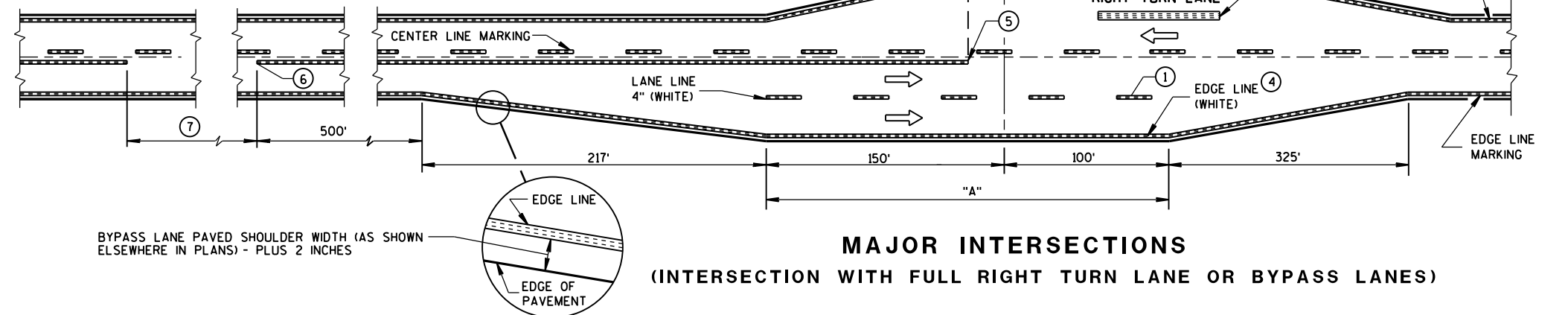
/S/ Travis Feltes
STATE TRAFFIC ENGINEER



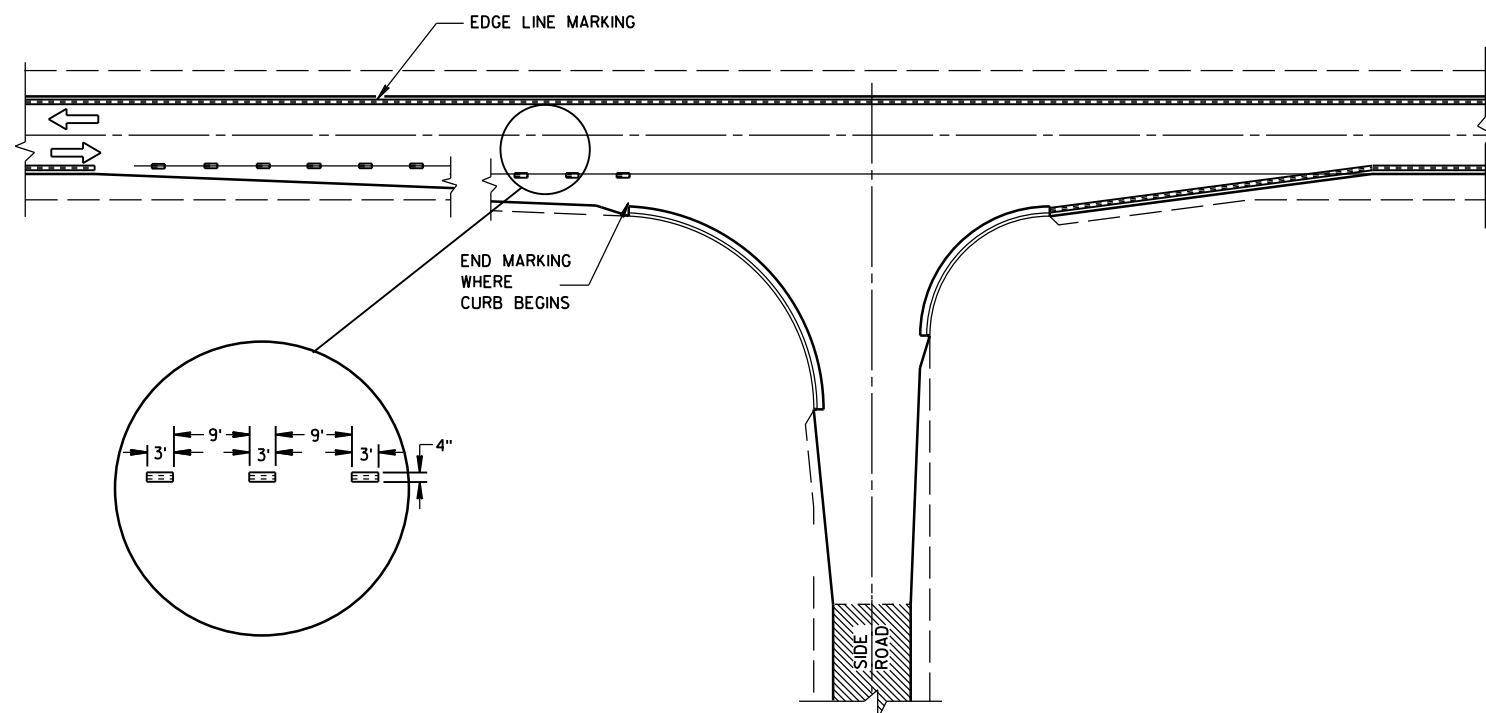
MINOR INTERSECTION WITHOUT CURBS

⑦

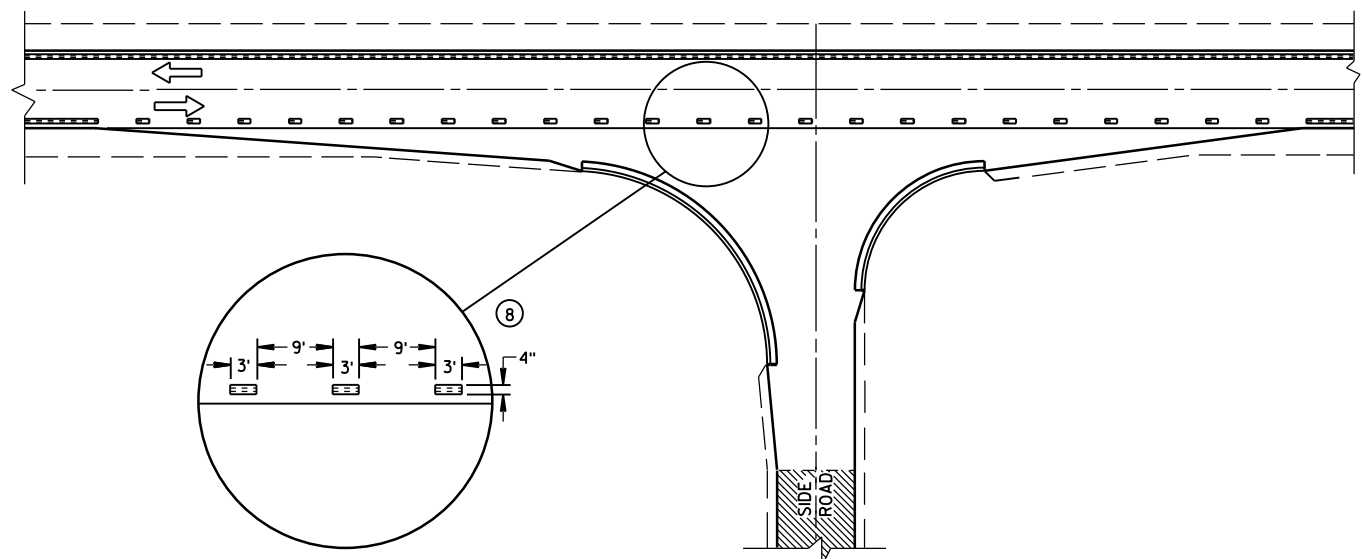
POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)



MINOR INTERSECTION WITH CURBS
(TYPICAL MARKING)



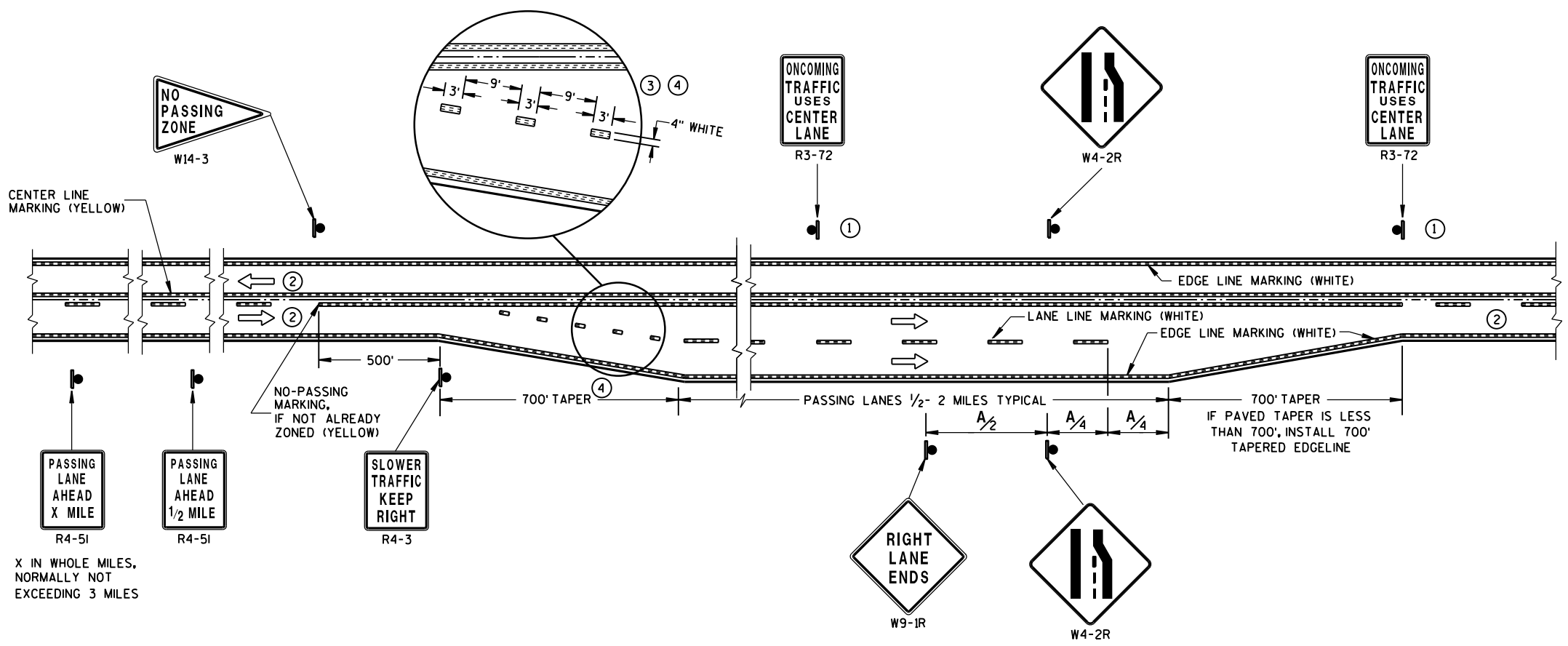
MINOR INTERSECTION WITH CURBS
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)

GENERAL NOTES

- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
 - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
 - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
 - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.
 - ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
 - ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
 - ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
 - ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

PAVEMENT MARKING
(INTERSECTIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**SOLID DOUBLE-YELLOW LINE
(THROUGHOUT ENTIRE PASSING/CLIMBING LANE)**

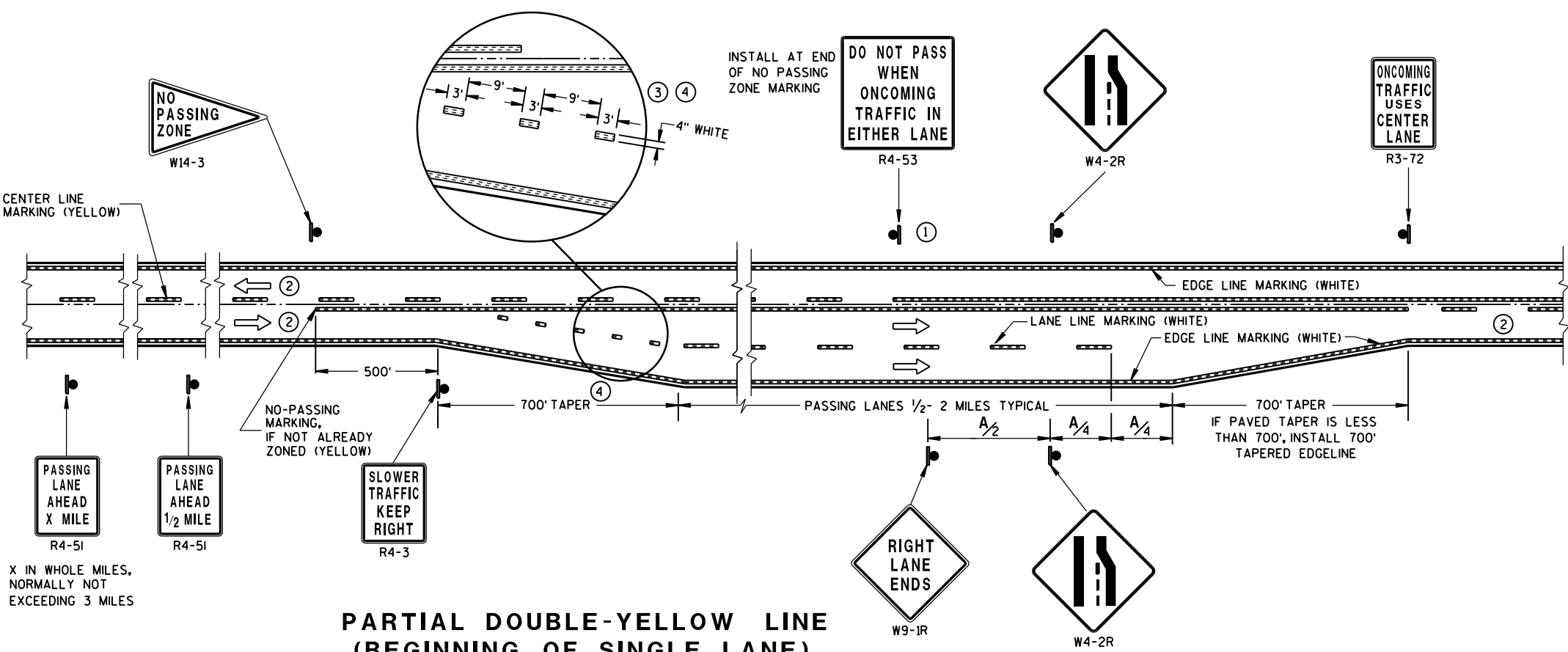
GENERAL NOTES

- ① SIGN SHALL BE REPEATED AT 1 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
- ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (➡) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

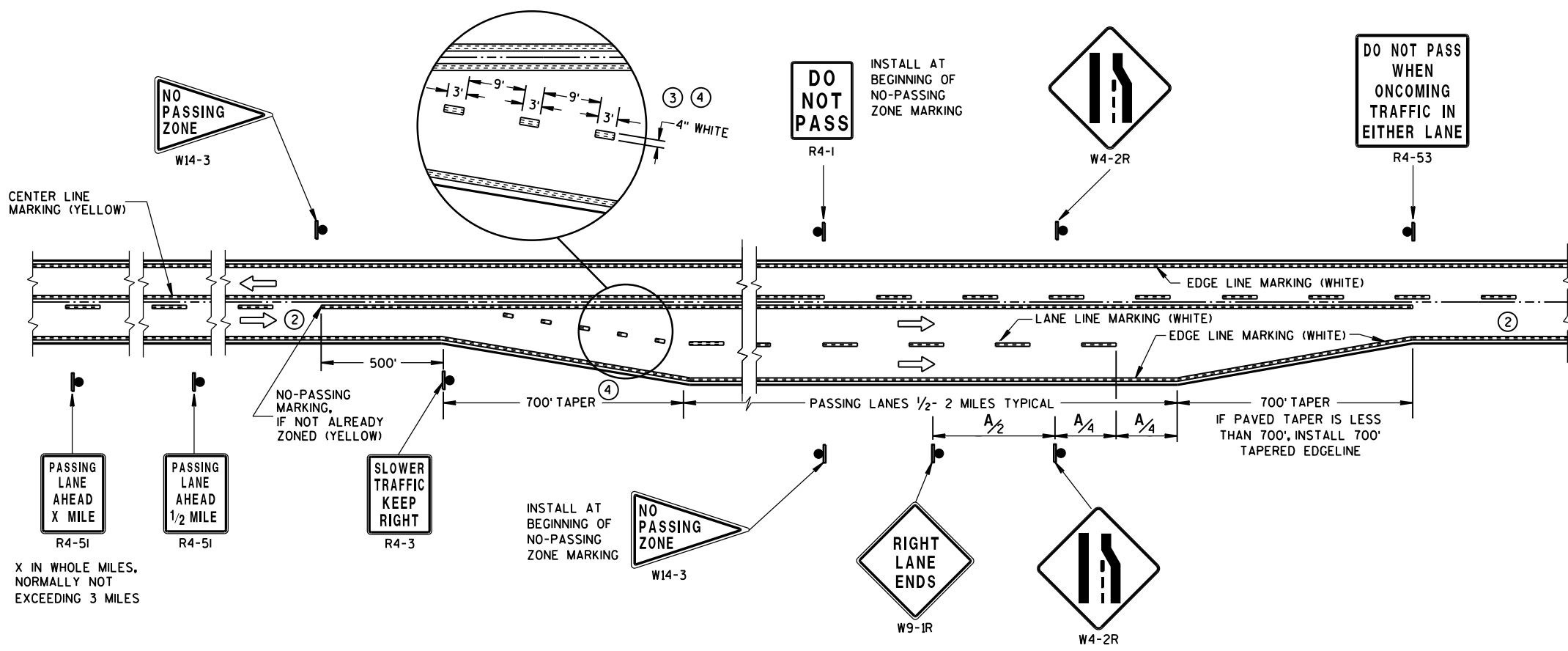
POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950



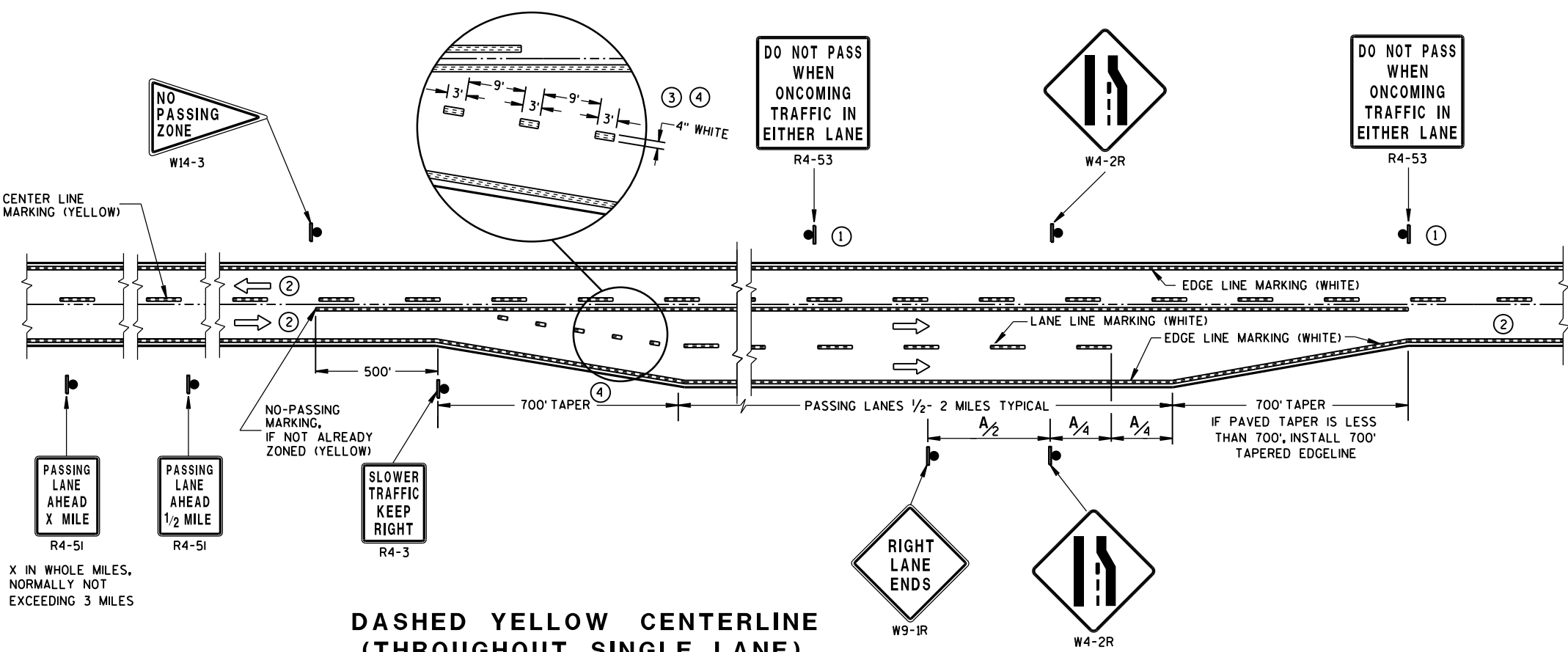
**PARTIAL DOUBLE-YELLOW LINE
(BEGINNING OF SINGLE LANE)**

**PAVEMENT MARKING & SIGNING
(CLIMBING LANE & PASSING LANE)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**SOLID DOUBLE-YELLOW LINE
(END OF SINGLE LANE)**

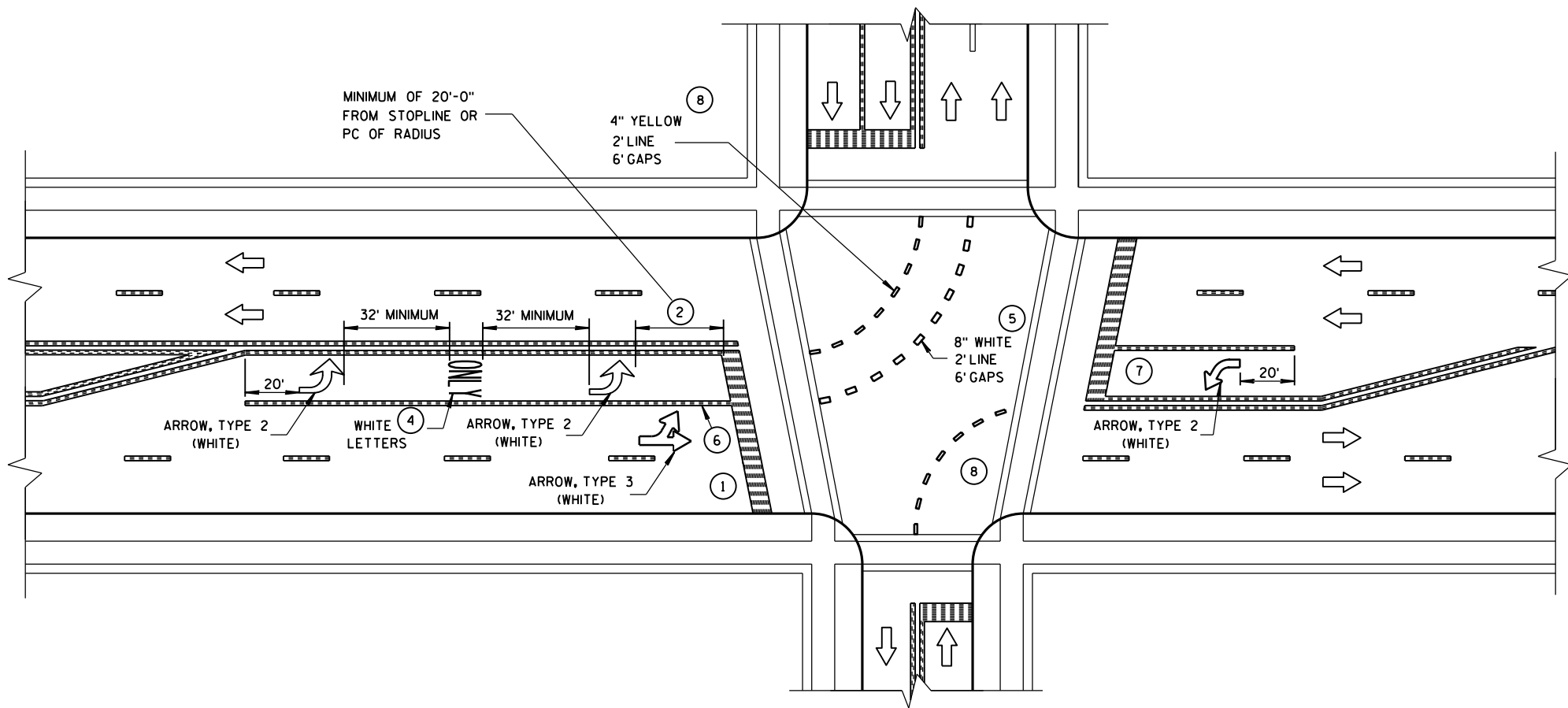


**DASHED YELLOW CENTERLINE
(THROUGHOUT SINGLE LANE)**

- GENERAL NOTES**
- ① SIGN SHALL BE REPEATED AT 1/2 MILE INCREMENTS OR AT THE DISCRETION OF THE REGIONAL TRAFFIC ENGINEER.
 - ② THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
 - ③ THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3" LINE 9" GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
 - ④ WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

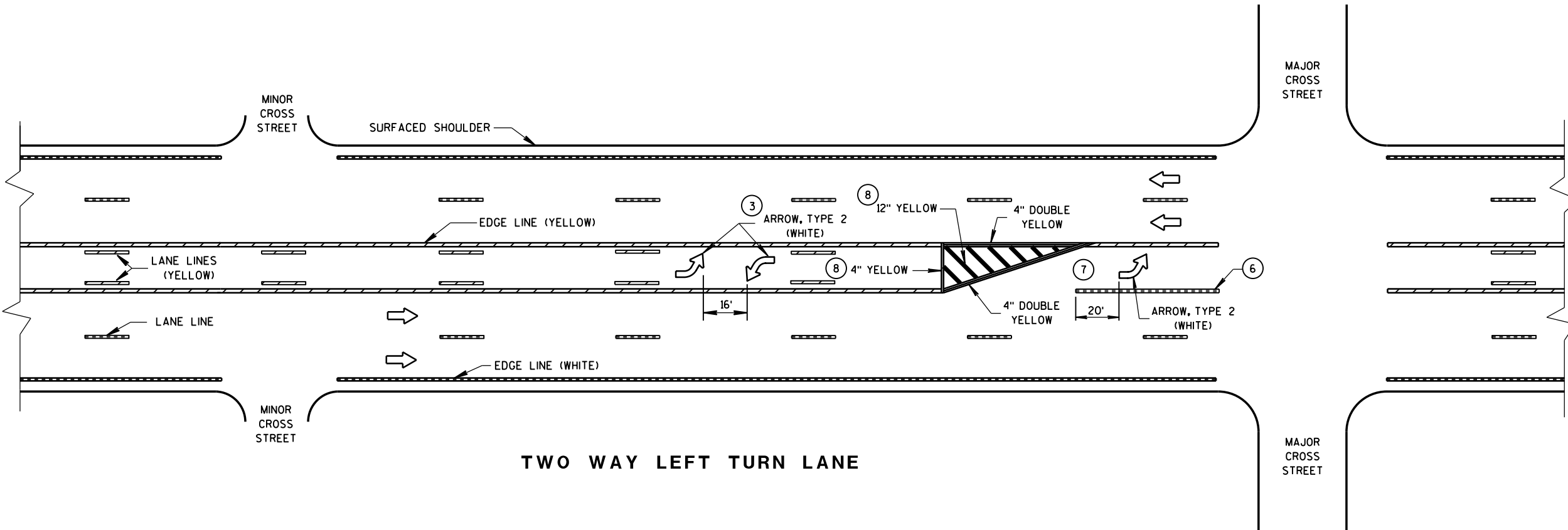
DISTANCE TABLE	
POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950



GENERAL NOTES

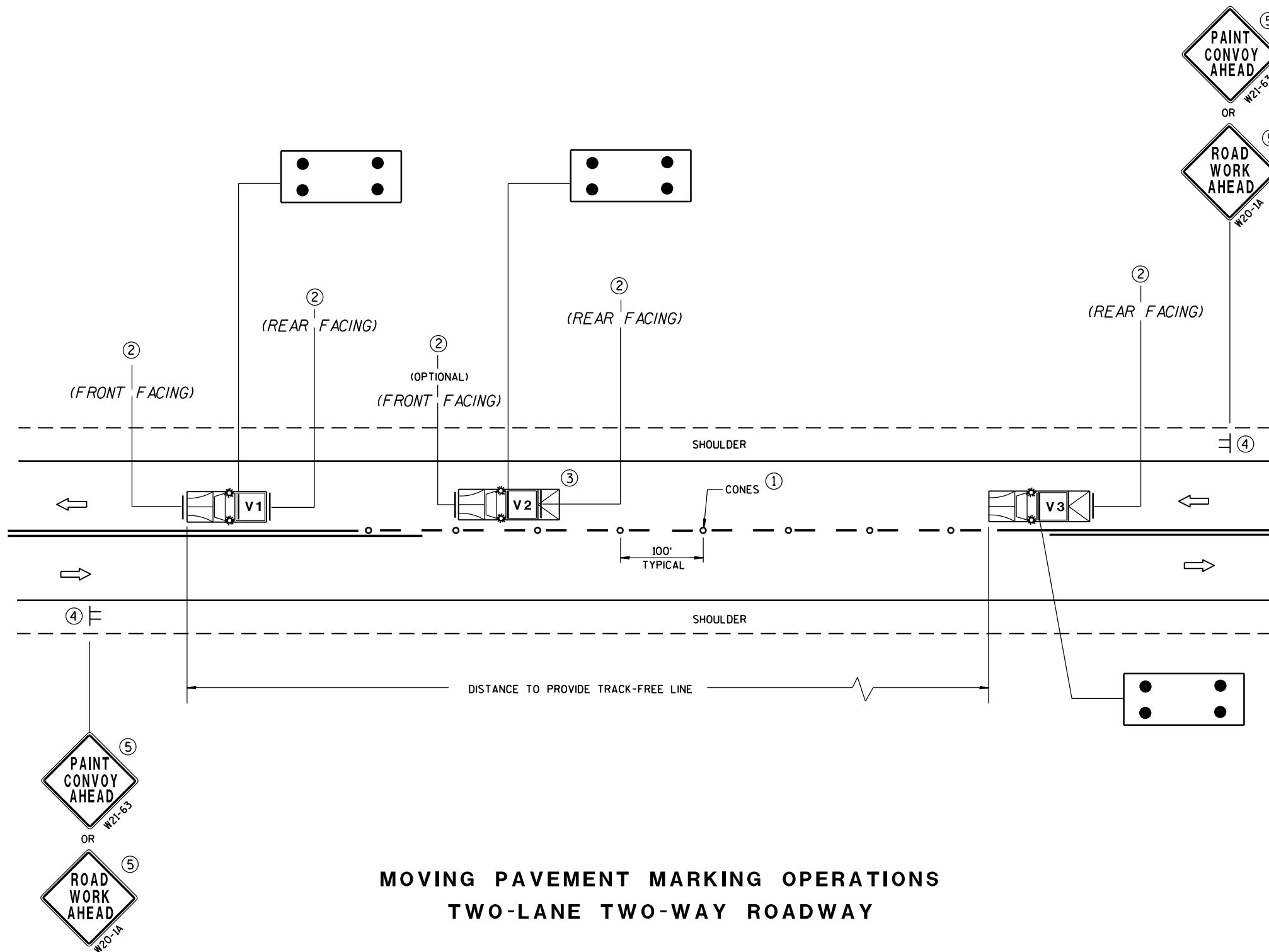
- 1 STOP BAR IS REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.
- 2 DISTANCE MAY BE ADJUSTED TO ACCOMODATE SHORT LEFT TURN LANES. AS APPROVED BY THE ENGINEER.
- 3 A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- 4 ADD EXTRA SETS OF ONE ARROW AND ONE ONLY PER 160 FEET OR WHEN ON A CURVE.
- 5 8" WHITE WITH 2' LINE 6' GAPS FOR DUAL TURN LANE.
- 6 8" WHITE
- 7 ADD SECOND ARROW WHEN TURN BAY IS GREATER THAN OR EQUAL TO 108 FEET.
- 8 REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.

NOTE:
ARROW SYMBOL (➡)
SHOWS DIRECTION OF TRAVEL



PAVEMENT MARKING
(LEFT TURN LANE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MOVING PAVEMENT MARKING OPERATIONS TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

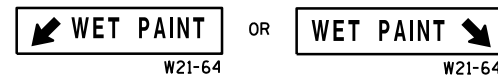
THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.



③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.

④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

V1 LEAD VEHICLE

V2 SHADOW VEHICLE

V3 TRAIL VEHICLE WITH TMA

TMA TRUCK-MOUNTED ATTENUATOR

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

CONES

FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING
OPERATION
TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

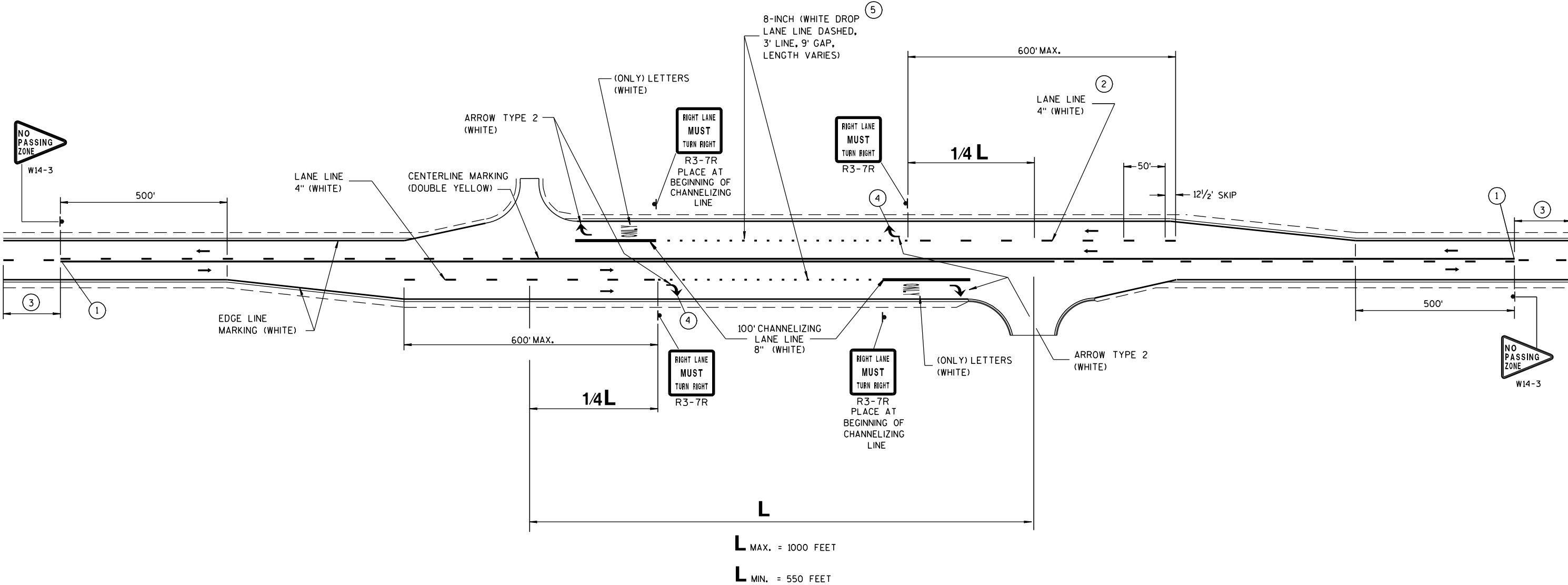
APPROVED

DATE /S/ Peter Amakobe Atepe
STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

FHWA

GENERAL NOTES

- 1 BARRIER LINE STARTS AT 500 FEET PRIOR TO THE BYPASS TAPER.
- 2 PLACE R3-7R SIGNS AT BEGINNING OF WHITE DROP LANE LINE DASHED AND WHITE CHANNEL LINE.
- 3 IF THE DISTANCE BETWEEN TWO SUCCESSIVE NO PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE TWO ZONES.
- 4 REQUIRED ONLY WHEN SPECIFIED IN THE PLANS.
- 5 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.



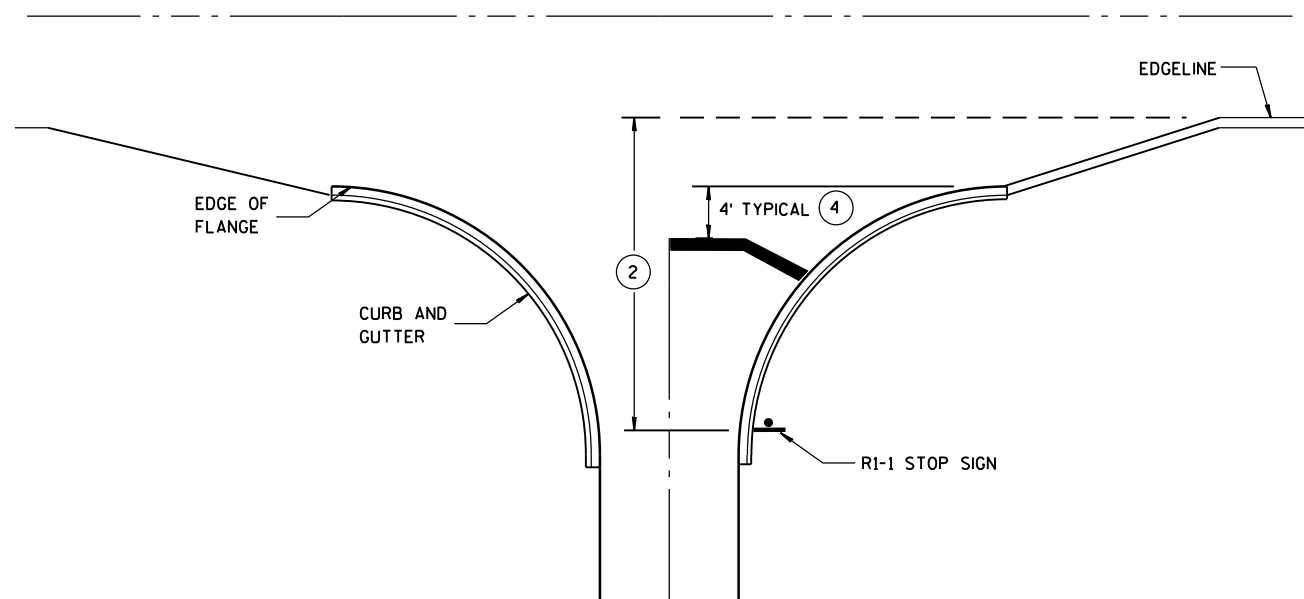
LEGEND

↔ DIRECTION OF TRAFFIC

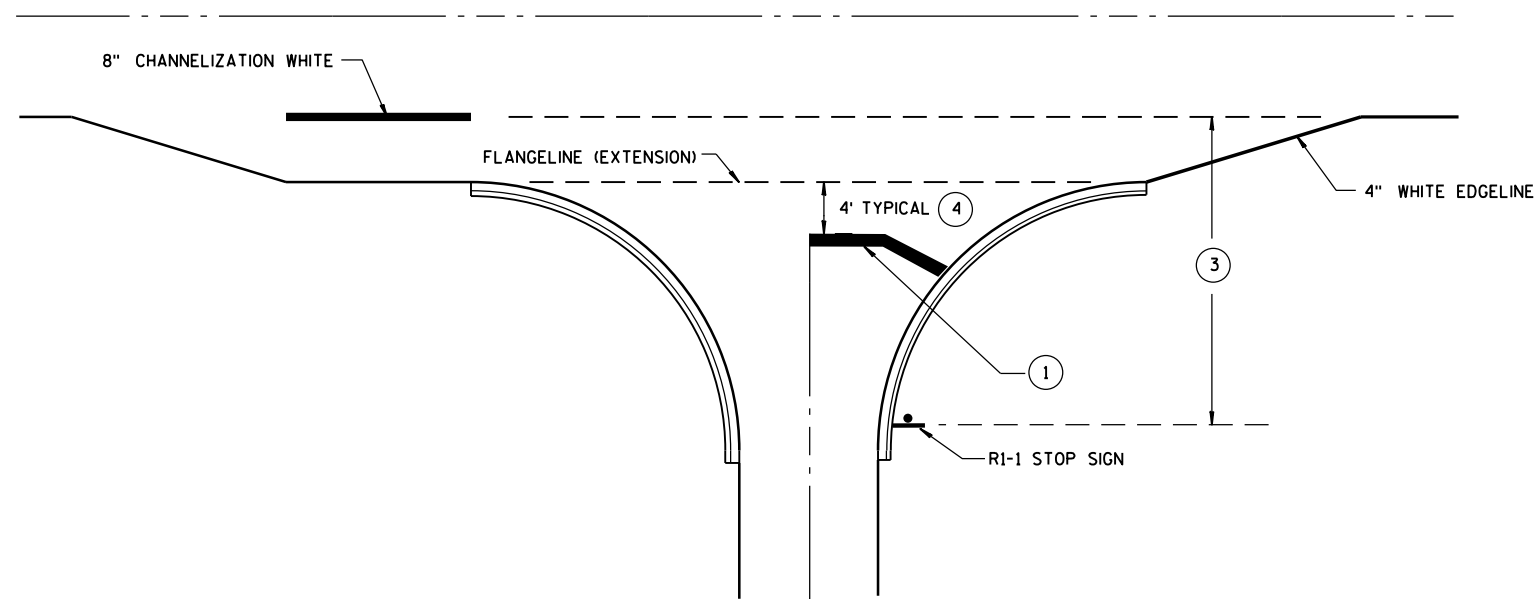
SIGNING AND MARKING
FOR COMBINATION RIGHT TURN
AND BYPASS LANE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

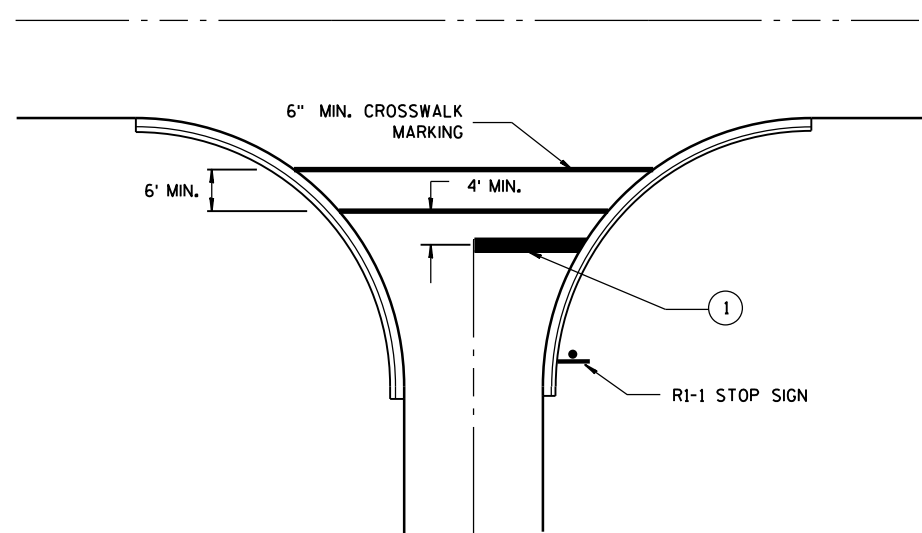
APPROVED
5-13-10 /S/ Thomas N. Notbohm
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



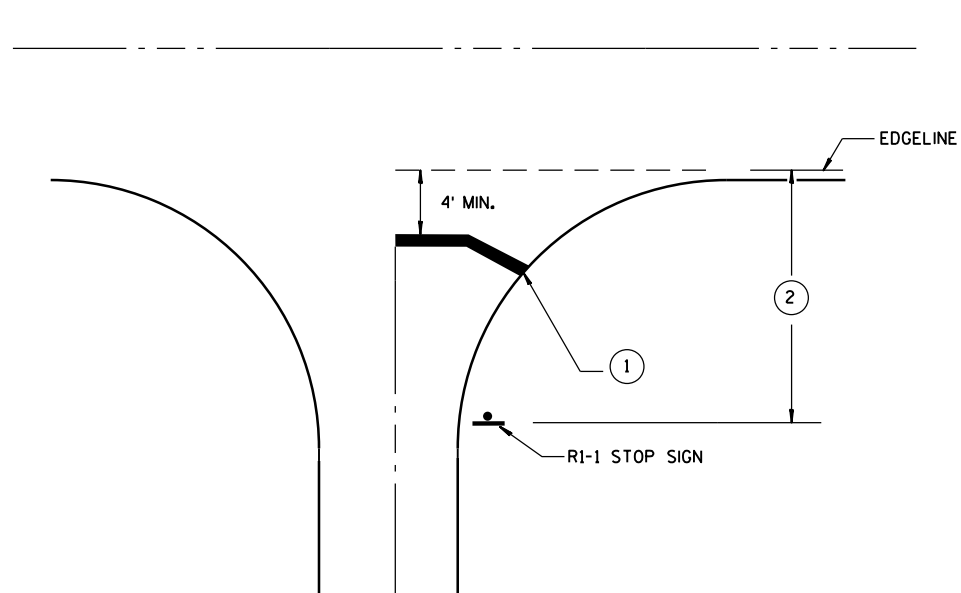
**TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER**



**TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH RIGHT TURN LANE**



**TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDEROADS WITH CROSSWALK MARKING**



**TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER**

GENERAL NOTES

- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGE LINE THAN NO STOP LINE IS REQUIRED.
- ③ IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
- ④ MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

STOP LINE AND CROSSWALK PAVEMENT MARKING

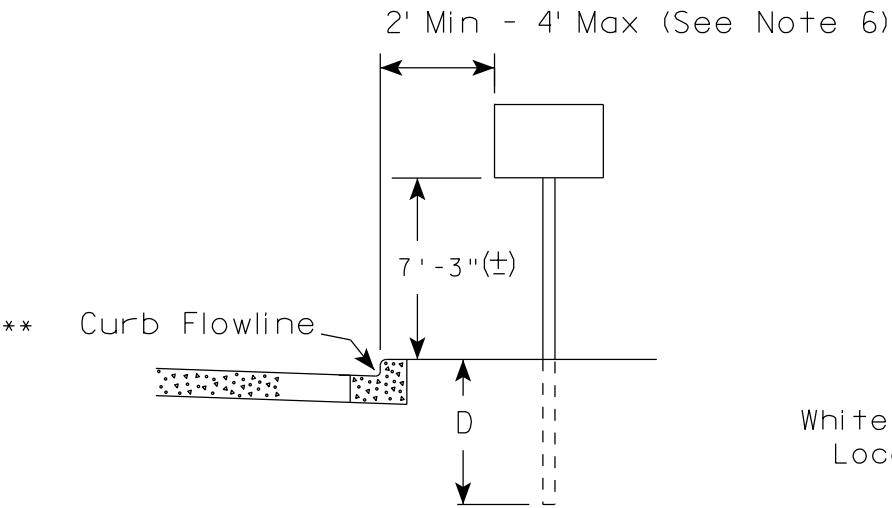
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4/30/2013
DATE

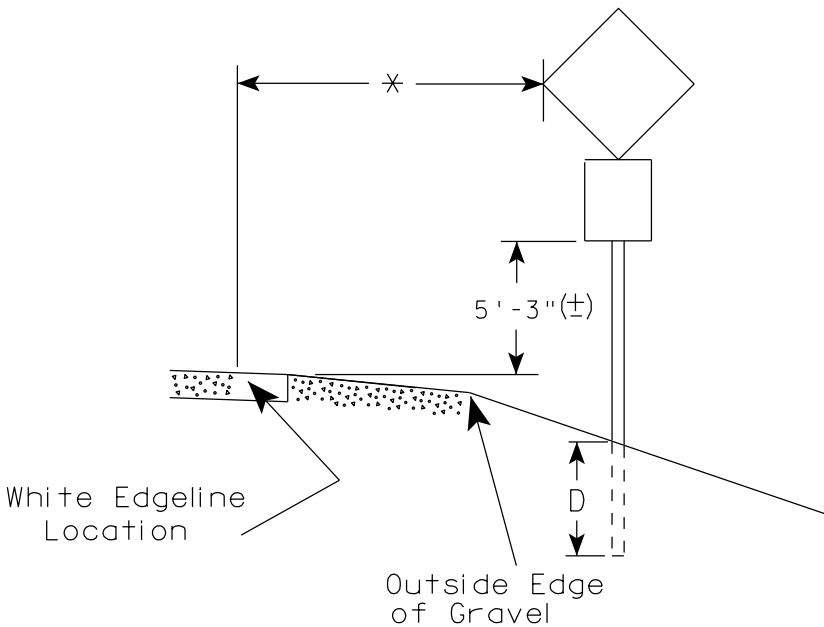
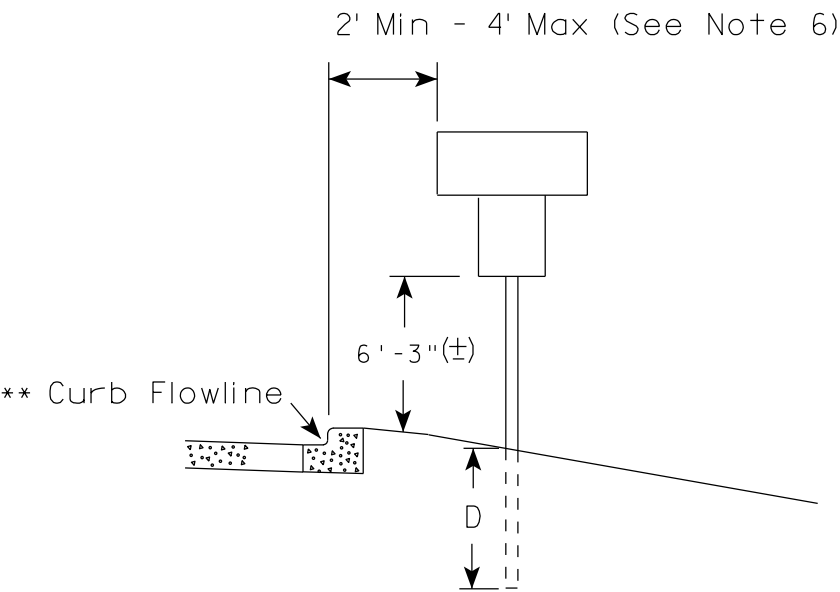
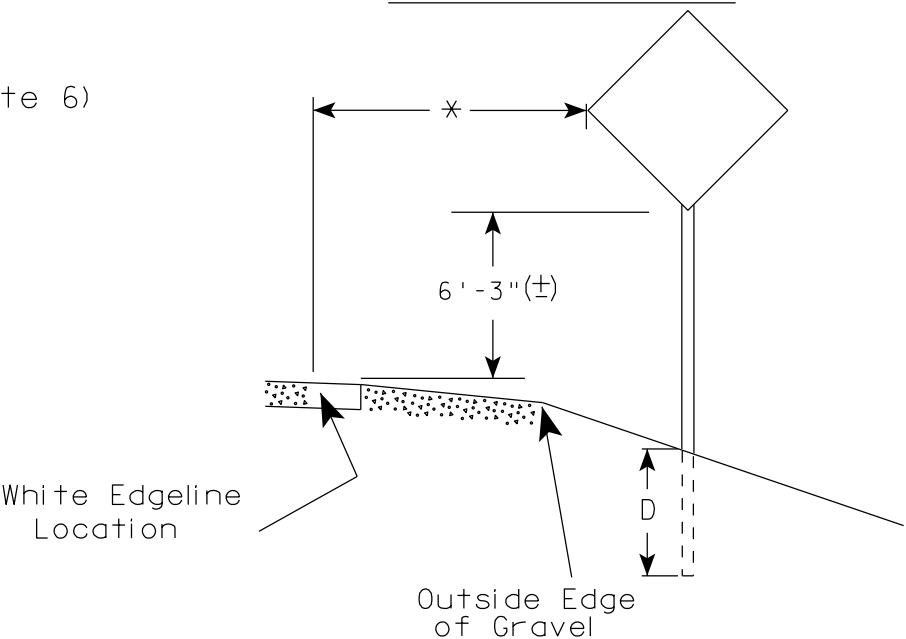
/S/ Travis Feltz
STATE TRAFFIC ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

×× The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

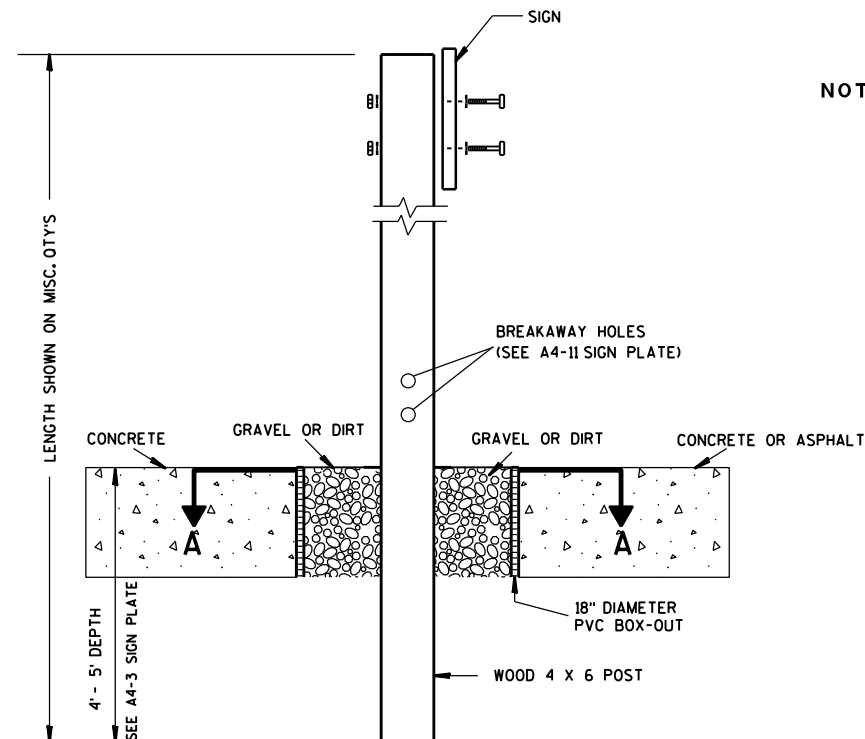
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

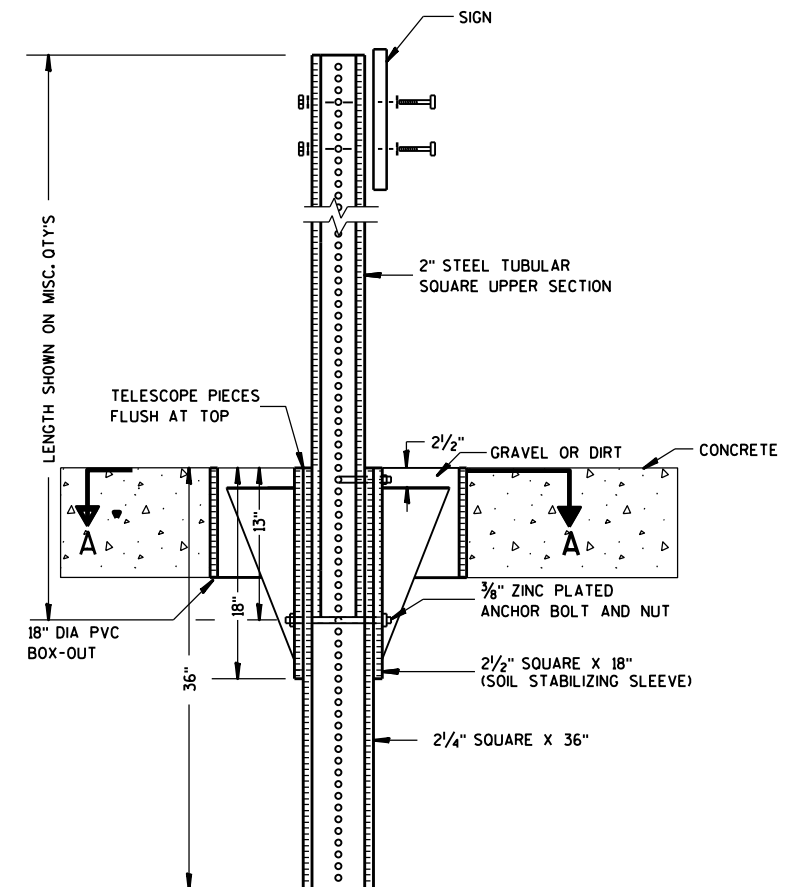
DATE 7/23/15 PLATE NO. A4-3.20



ELEVATION VIEW

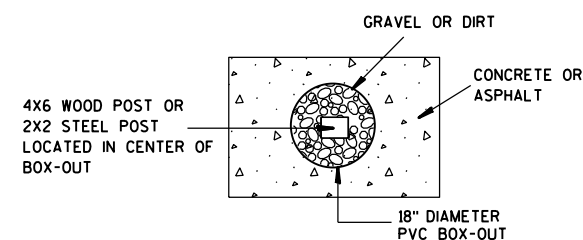
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES: 1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST
BOX-OUTS
A4-3B

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

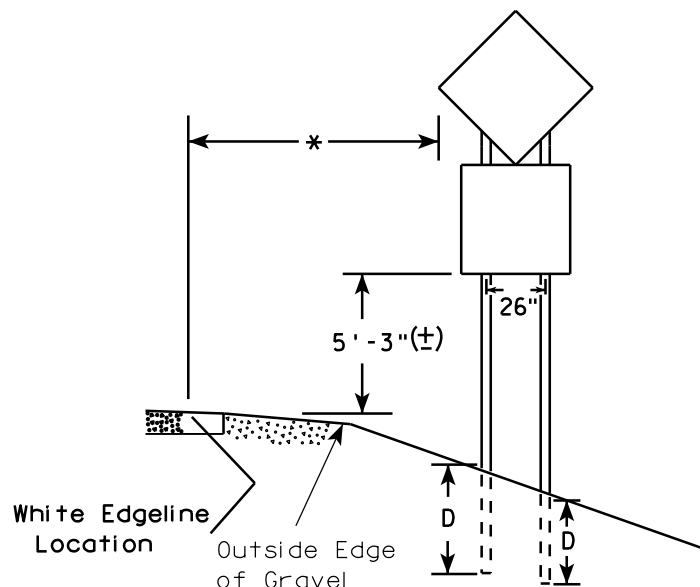
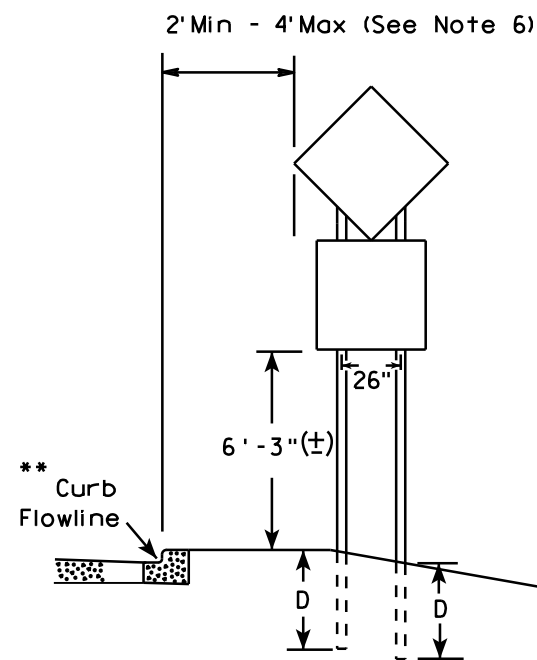
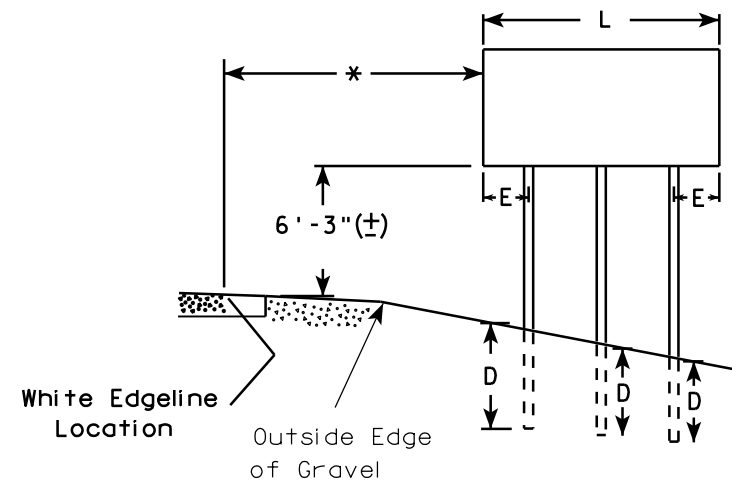
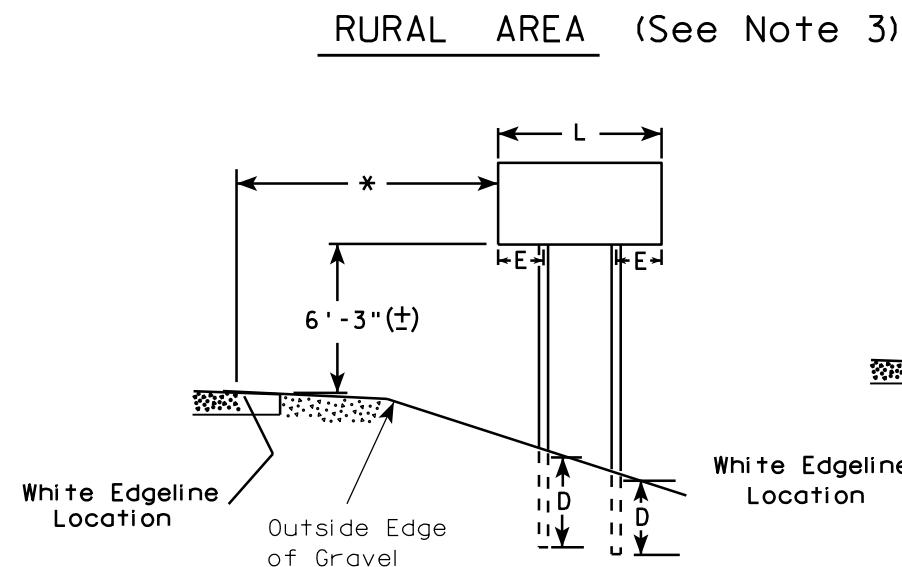
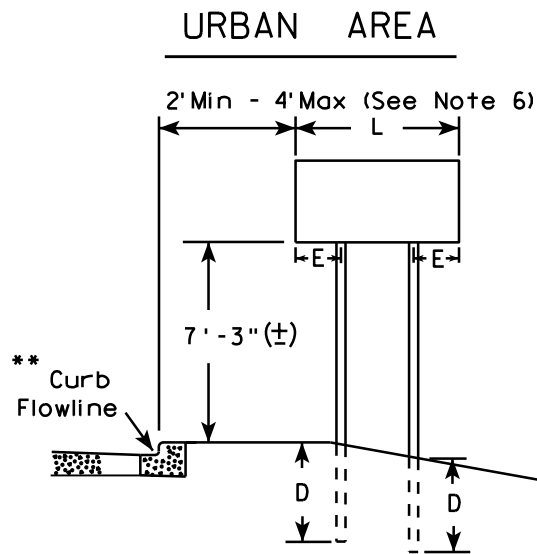
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



48" DIAMOND WARNING SIGN

48" DIAMOND WARNING SIGN

- GENERAL NOTES**
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
 2. See tables below for required number of posts.
 3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
 4. The (±) tolerance for mounting height is 3 inches.
 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

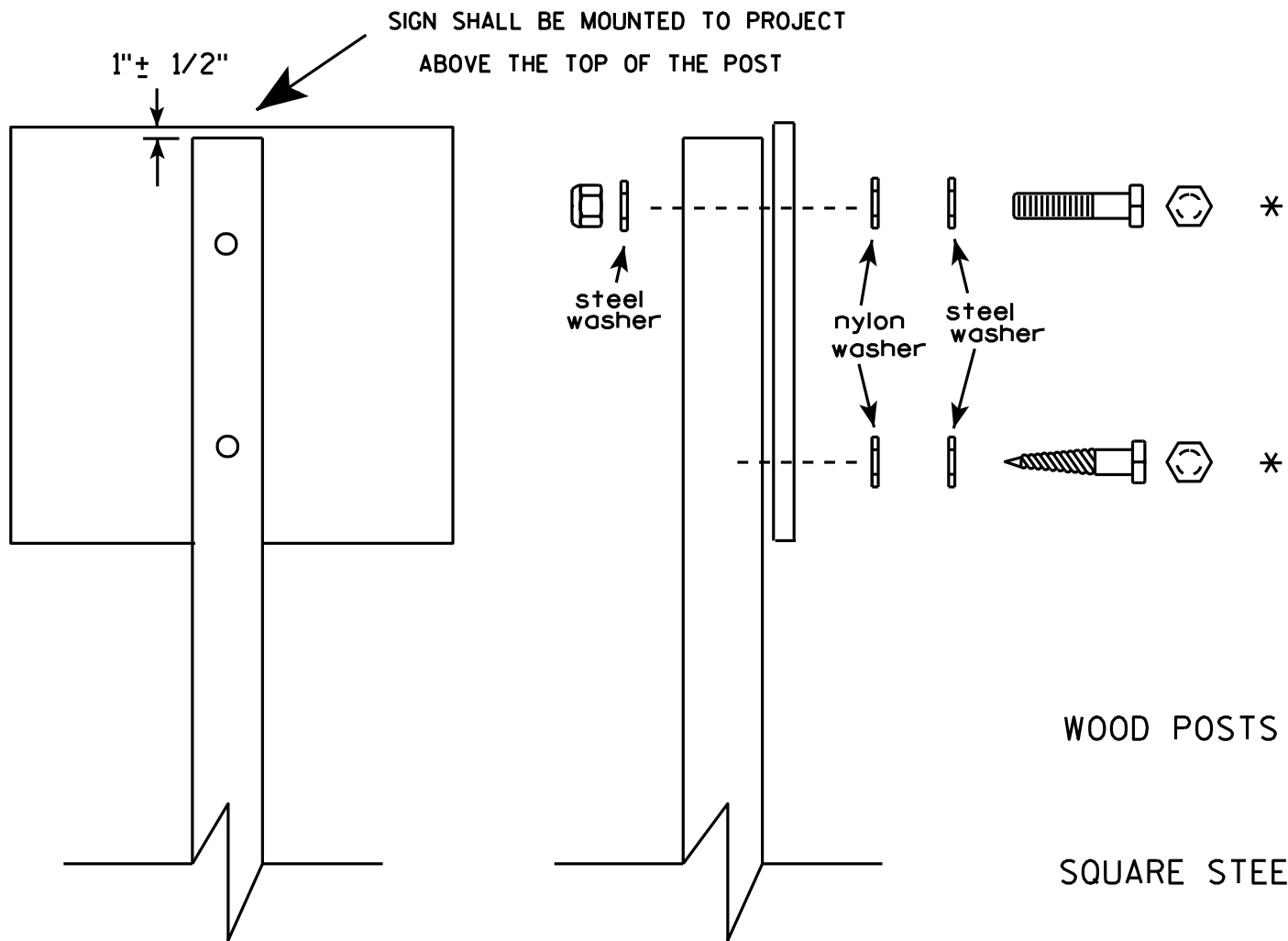
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/23/15 PLATE NO. A4-4.14

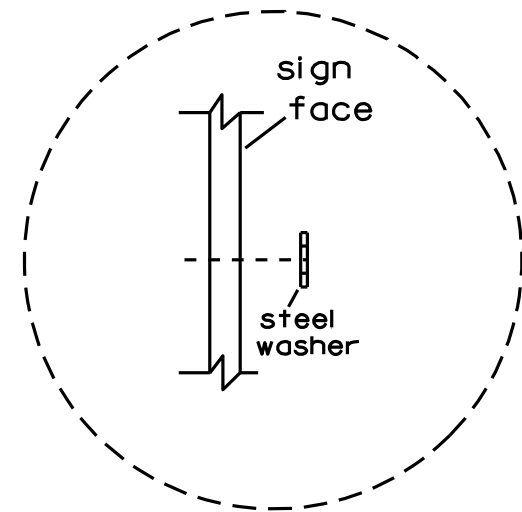


Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- WOOD POSTS (4" x 4" or 4" x 6")
LAG SCREWS - 3/8" X 3"
MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts
- SQUARE STEEL POSTS (2" x 2")
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.

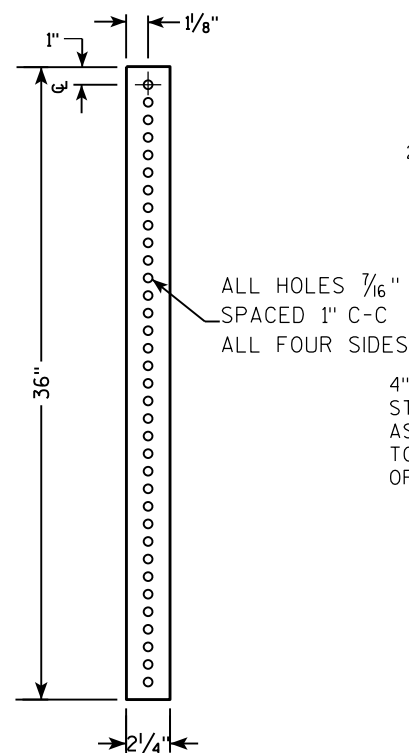


Washer Placement when Sign Has Other Than Type H or Type F Face

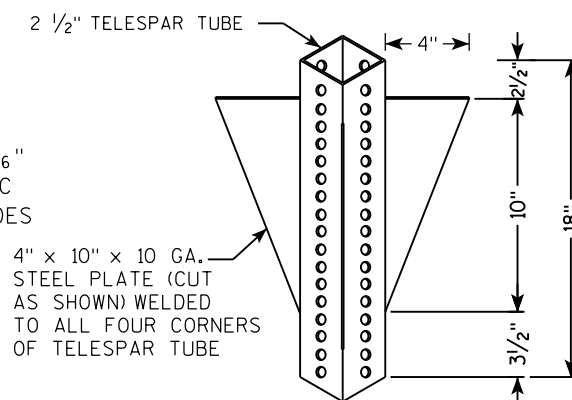
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/23/10	PLATE NO. A4-8.7

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



TECHNICAL DRAWING OF A VERTICAL SIGN POST ASSEMBLY.

Labels and Dimensions:

- 18" DIA SCHEDULE 40 PVC BOX-OUT**: The base container for the post.
- 36"**: Total height of the post assembly.
- 18"**: Height of the upper section.
- 13"**: Height of the lower section.
- 2 1/2" GRAVEL OR DIRT**: Material filling the box-out.
- 2 1/4" SQUARE X 36"**: The main vertical post.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: Sleeve around the post in the lower section.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Bolt securing the sleeve.
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT**: Bolt securing the post to the sleeve.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The top part of the post.
- ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES**: Specification for the post's perforations.
- SIGN**: The sign plate at the top.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to a sign plate for hardware details.
- TELESCOPE PIECES FLUSH AT TOP**: Note on the top connection.

TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY.

Side View Dimensions:

- Overall height: LENGTH SHOWN ON MISC. Q'TYS
- Top section height: 8"
- Section below top: 18"
- Section below that: 12"
- Section below that: 36"

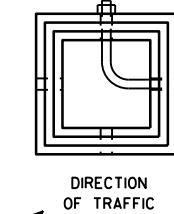
End View Dimensions:

- Top section width: 2"
- Section below top: 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)
- Section below that: 2 1/4" SQUARE X 36"

Labels and Notes:

- SIGN
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL
- 2" STEEL TUBULAR SQUARE UPPER SECTION
- ALL HOLES 7/16" SPACED 1" C-C
- ALL FOUR SIDES
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT
- TELESCOPE PIECES FLUSH AT TOP
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT

3/8" ZINC PLATED CORNER
ANCHOR BOLT AND NUT



SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

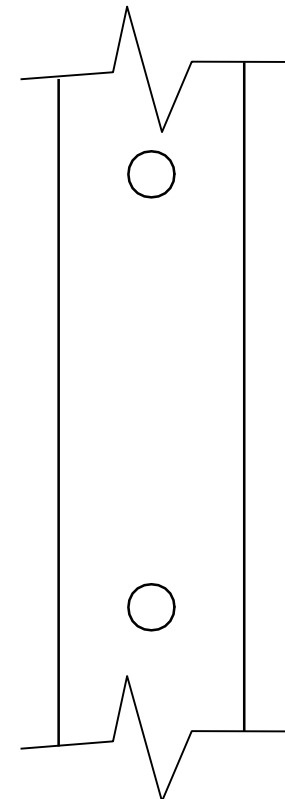
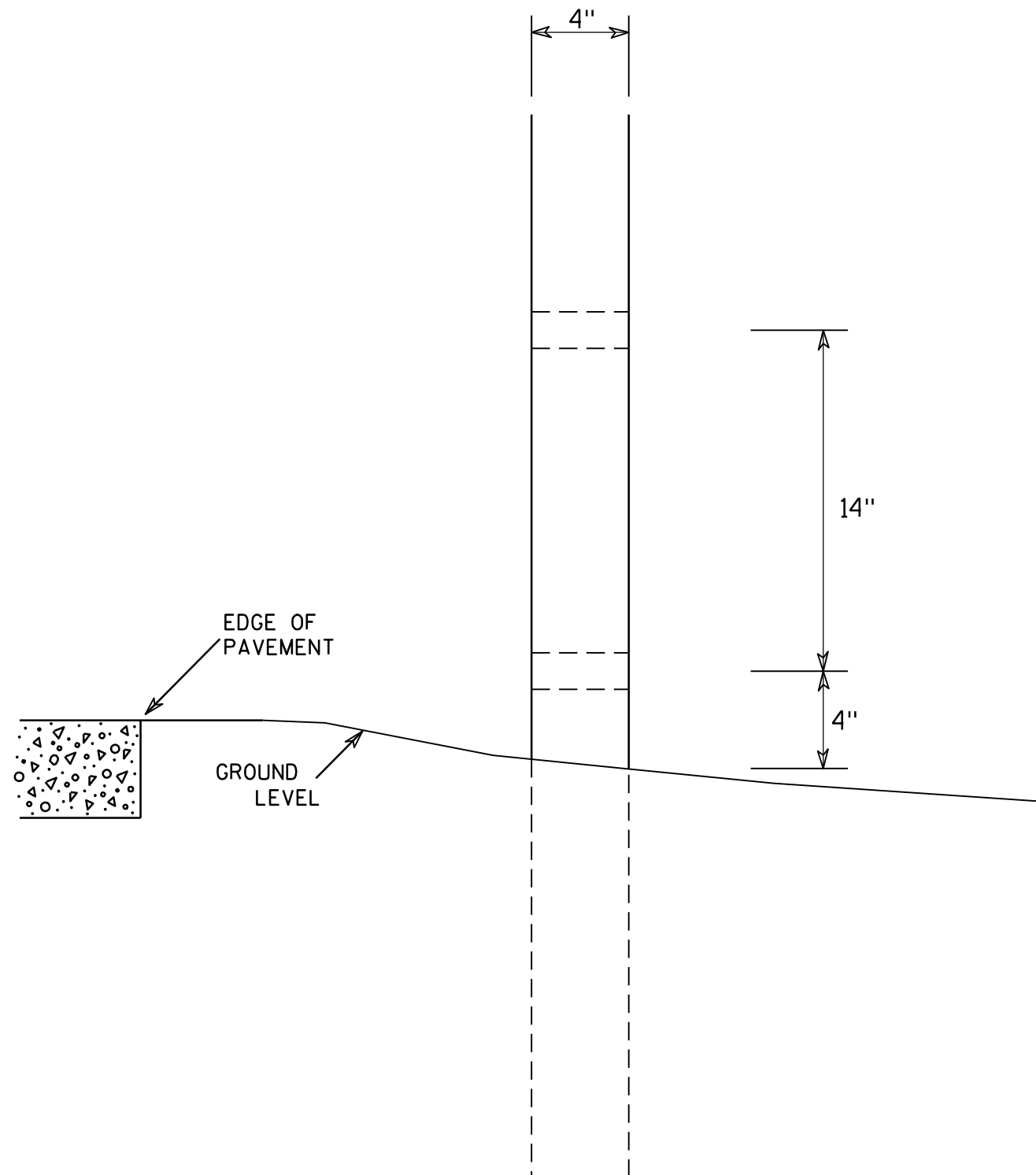
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1 1/2" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

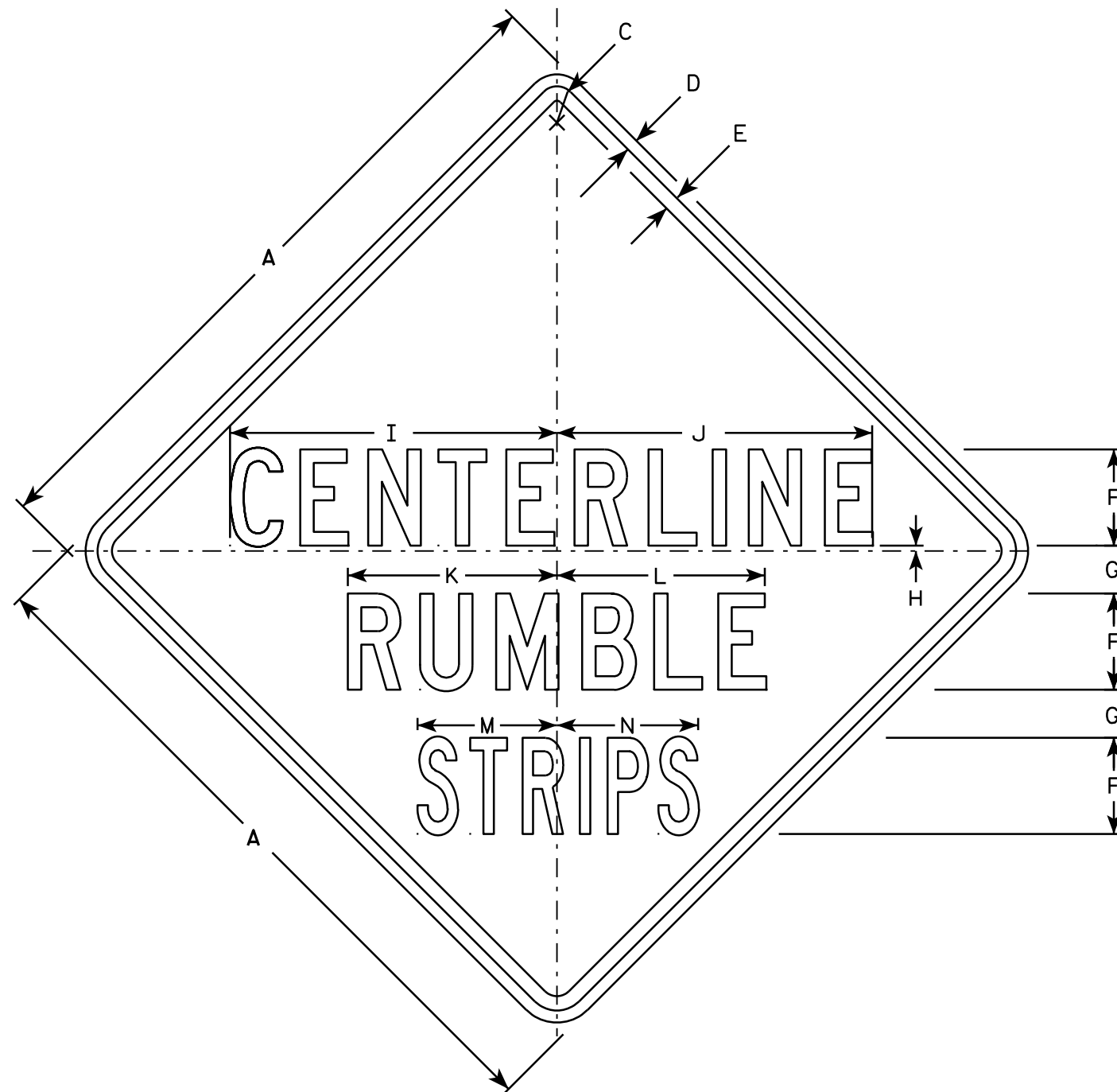
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



W8-70

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - YELLOW
Message - BLACK
3. Message Series - SEE NOTE 5.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 and 2 are Series C.
Line 3 is Series B.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5/8	5/8	3/4	5	2 1/2	1/4	17	16 3/8	10 7/8	10 3/4	7 1/4	7 3/8													9.0
2M	36		1 5/8	5/8	3/4	5	2 1/2	1/4	17	16 3/8	10 7/8	10 3/4	7 1/4	7 3/8													9.0
3	36		1 5/8	5/8	3/4	5	2 1/2	1/4	17	16 3/8	10 7/8	10 3/4	7 1/4	7 3/8													9.0
4																											
5																											

STANDARD SIGN

W8-70

WISCONSIN DEPT OF TRANSPORTATION

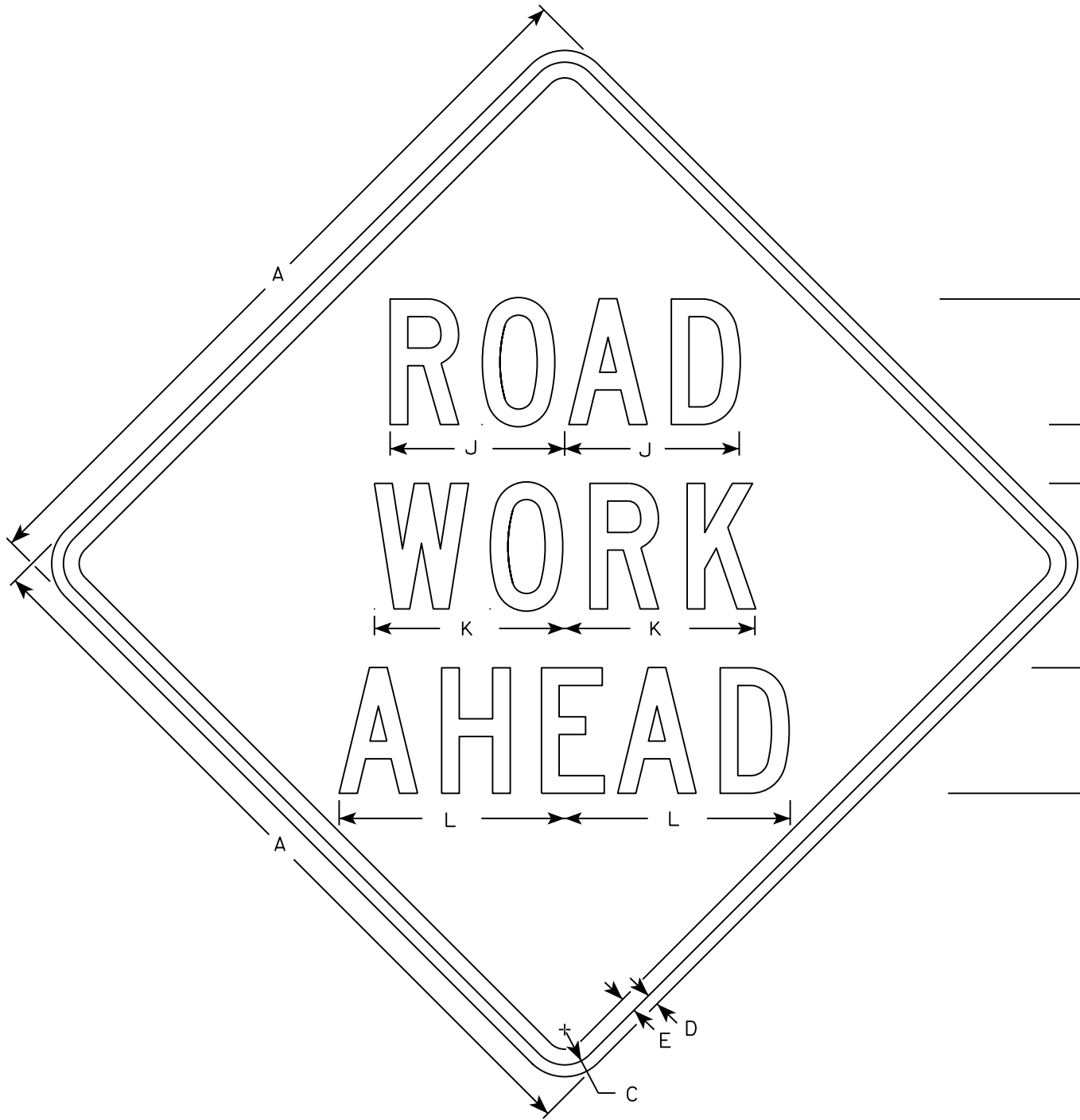
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 03/18/13 PLATE NO. W8-70.3

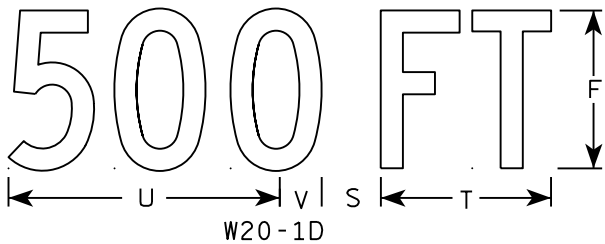
PROJECT NO:

SHEET NO:

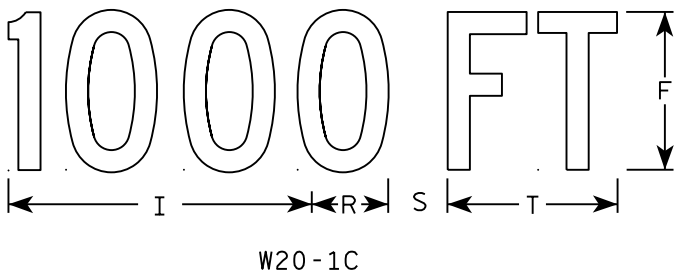
E



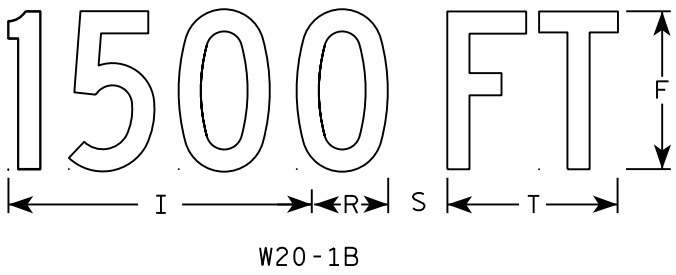
W20-1A



W20-1D



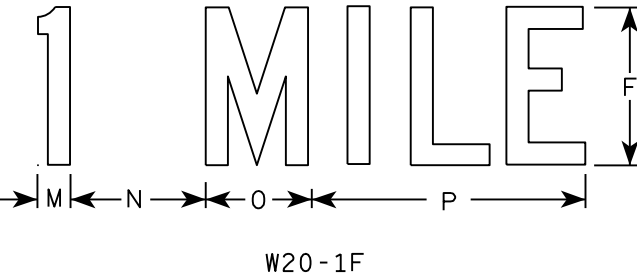
W20-1C



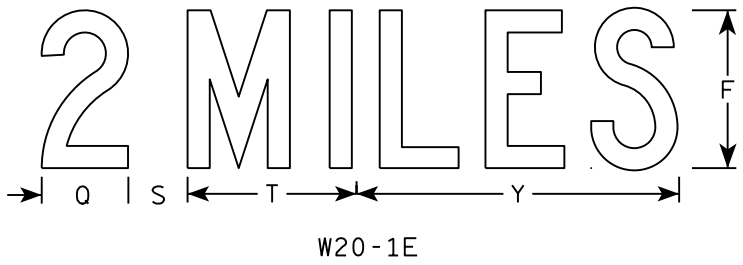
W20-1B



W20-1G



W20-1F



W20-1E

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

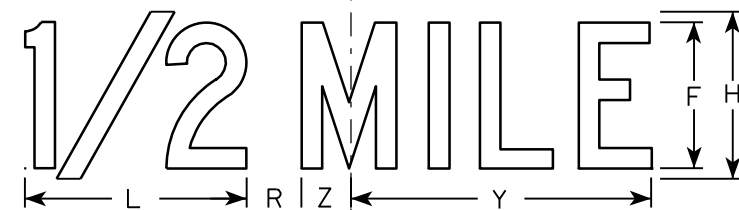
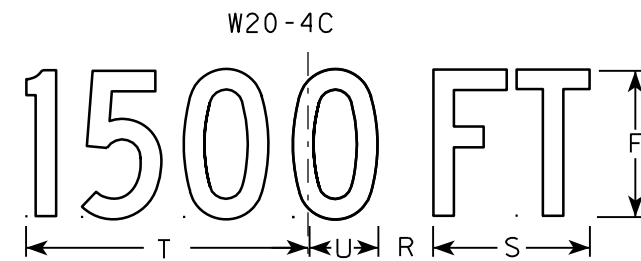
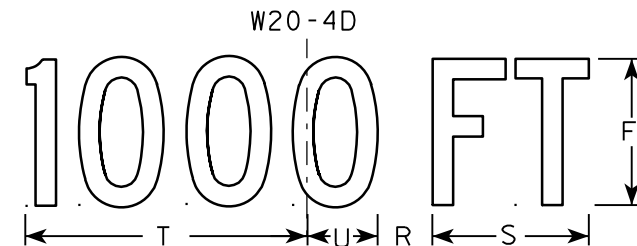
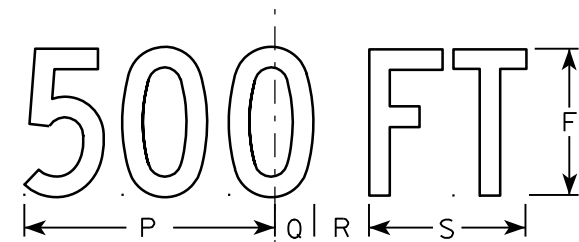
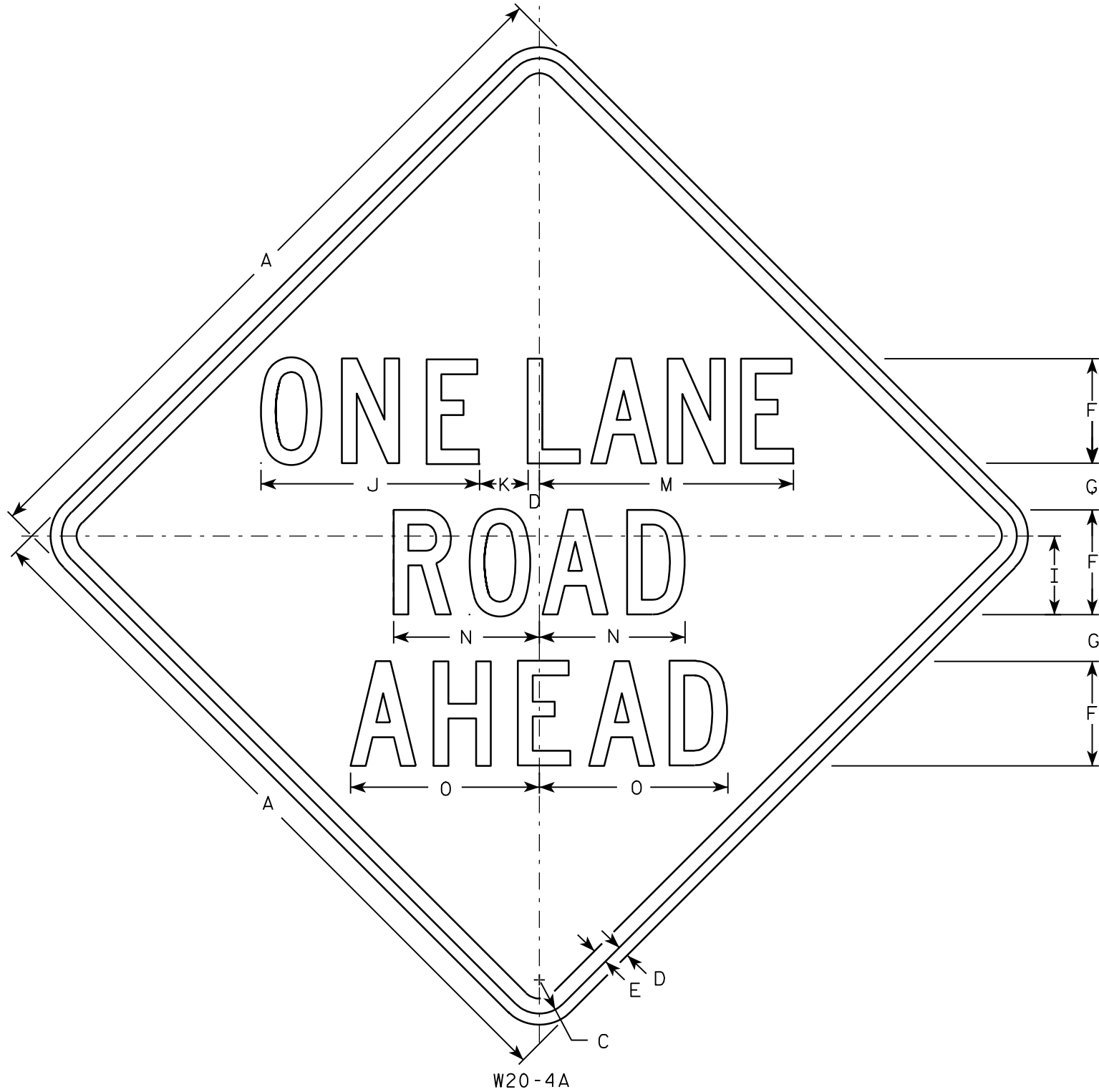
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/07/15 PLATE NO. W20-1.10



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

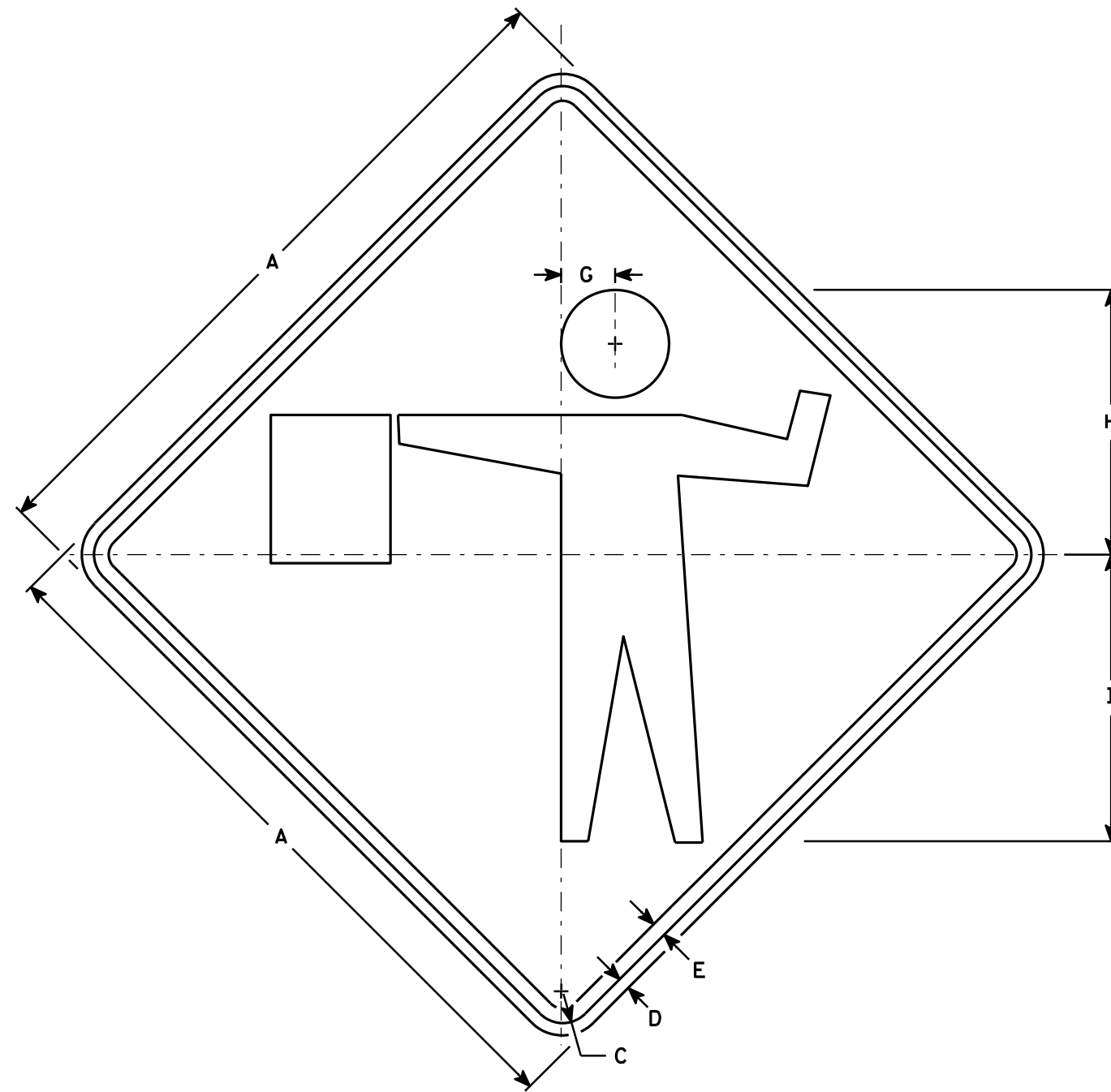
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 3/8	6	3 3/4	10 3/8	2 3/8	8	13 1/2	7	8 7/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	3 1/8	8	5 1/4	14 5/8	3 1/4	10 5/8	17 3/4	9 3/4	12 5/8	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-4A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-4.9



W20-7A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4		2 3/4	13 1/2	14 5/8																		9.00
2S	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
2M	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
3	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
4	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00
5	48		2 1/4	3/4	1		3 3/4	18	19 1/2																		16.00

STANDARD SIGN
W20-7A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-7A.5

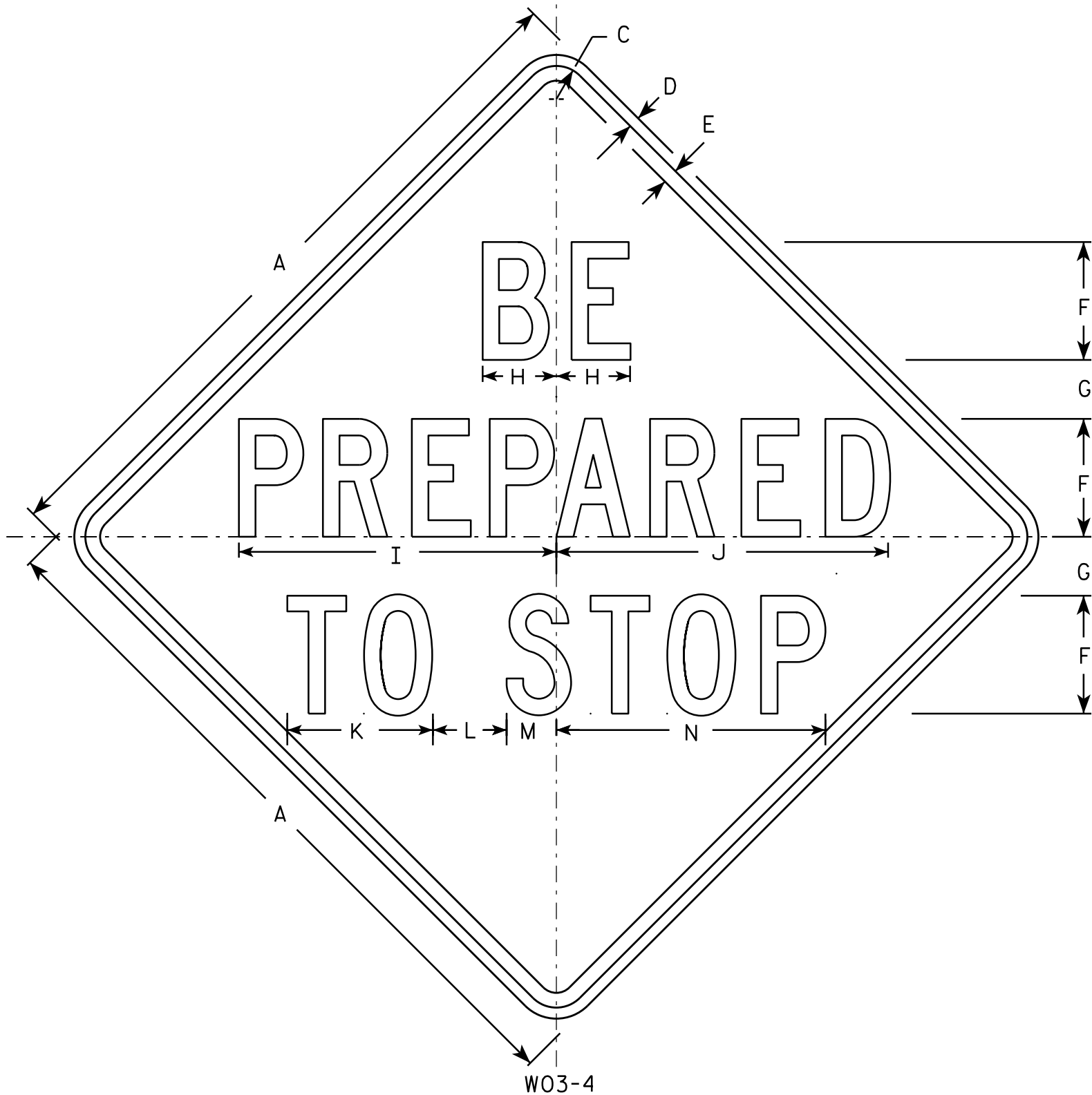
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3⁄8	1⁄2	5⁄8	6	2 1⁄2	3 3⁄4	15 7⁄8	16 7⁄8	7 3⁄8	4	2 3⁄8	13 3⁄4													9.0
2S	48		2 1⁄4	3⁄4	1	8	4	5	21 1⁄2	22 1⁄2	9 7⁄8	5	3 3⁄8	18 1⁄4													16.0
2M	48		2 1⁄4	3⁄4	1	8	4	5	21 1⁄2	22 1⁄2	9 7⁄8	5	3 3⁄8	18 1⁄4													16.0
3	48		2 1⁄4	3⁄4	1	8	4	5	21 1⁄2	22 1⁄2	9 7⁄8	5	3 3⁄8	18 1⁄4													16.0
4	48		2 1⁄4	3⁄4	1	8	4	5	21 1⁄2	22 1⁄2	9 7⁄8	5	3 3⁄8	18 1⁄4													16.0
5	48		2 1⁄4	3⁄4	1	8	4	5	21 1⁄2	22 1⁄2	9 7⁄8	5	3 3⁄8	18 1⁄4													16.0

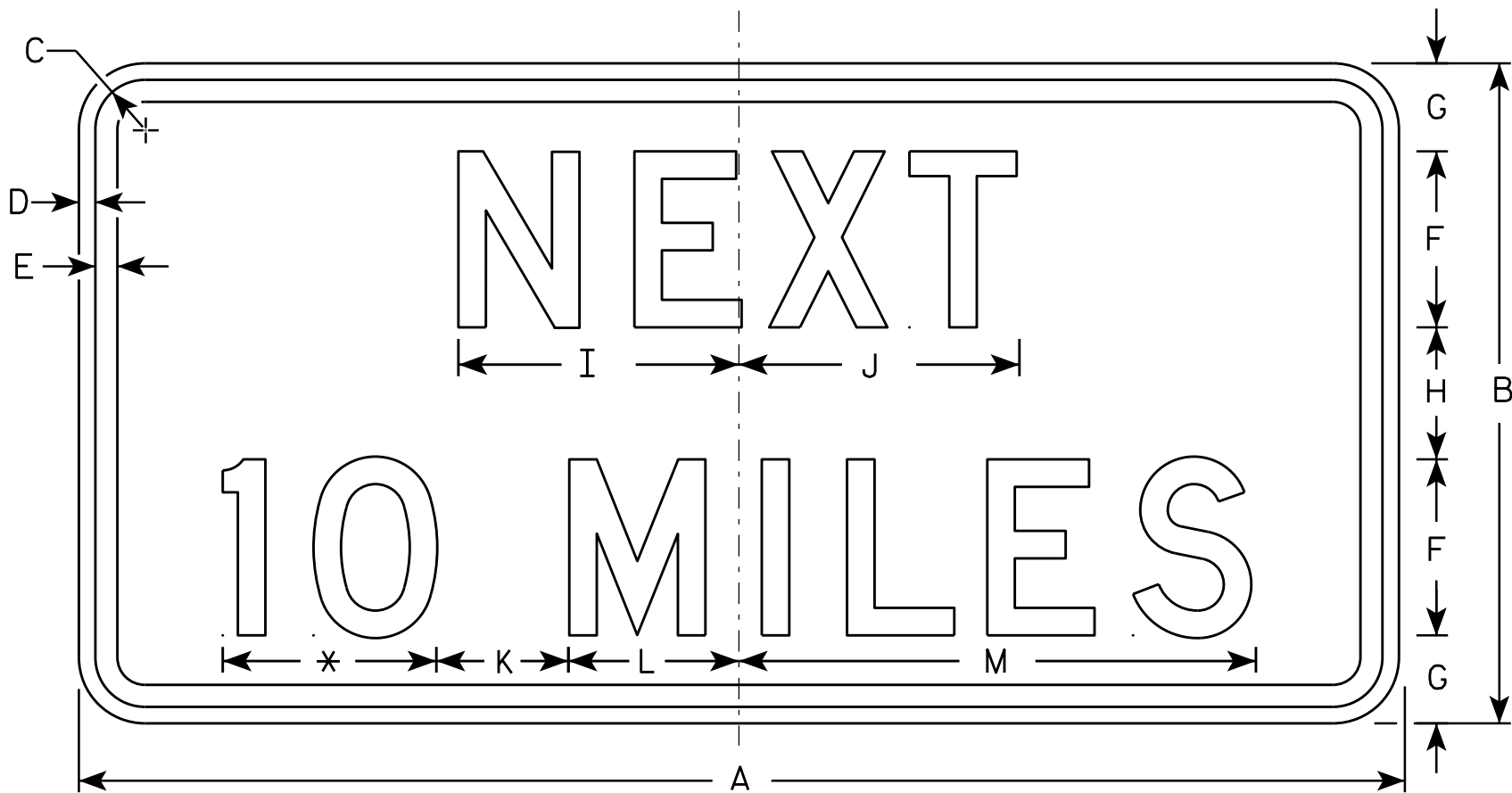
STANDARD SIGN
W03-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/02/13 PLATE NO. W03-4.1

PROJECT NO: HWY: COUNTY: SHEET NO: E



W057-51

- NOTES
- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
 - 2. Color:
Background - Orange
Message - Black
 - 3. Message Series - D
 - 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
 - 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

* See note 5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 5/8	2 3/4	7 7/8	8	5	4 1/8	15 3/8														4.5
2S	48	24	1 3/8	1/2	5/8	6	3 1/2	5	10	10 1/8	6	5 5/8	19														8.0
2M	48	24	1 3/8	1/2	5/8	6	3 1/2	5	10	10 1/8	6	5 5/8	19														8.0
3	48	24	1 3/8	1/2	5/8	6	3 1/2	5	10	10 1/8	6	5 5/8	19														8.0
4	48	24	1 3/8	1/2	5/8	6	3 1/2	5	10	10 1/8	6	5 5/8	19														8.0
5	48	24	1 3/8	1/2	5/8	6	3 1/2	5	10	10 1/8	6	5 5/8	19														8.0

STANDARD SIGN
W057-51

WISCONSIN DEPT OF TRANSPORTATION

APPROVED
Matthew R. Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE NO. W057-51.1

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>