

MAD APRIL 2016

PROJECT ID: 1206-04-65

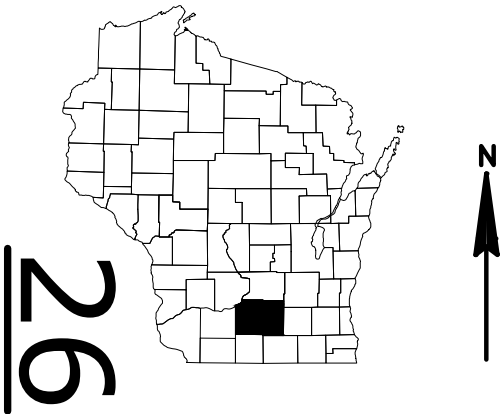
WITH: N/A

COUNTY: DANE

ORDER OF SHEETS

- Section No. 1 Title
- Section No. 2 Typical Sections and Details
- Section No. 3 Estimate of Quantities
- Section No. 3 Miscellaneous Quantities
- ~~Section No. 4 Right of Way Plat~~
- Section No. 5 Plan and Profile
- Section No. 6 Standard Detail Drawings
- ~~Section No. 7 Sign Plates~~
- Section No. 8 Structure Plans
- ~~Section No. 9 Computer Earthwork Data~~
- ~~Section No. 9 Cross Sections~~

TOTAL SHEETS = 74



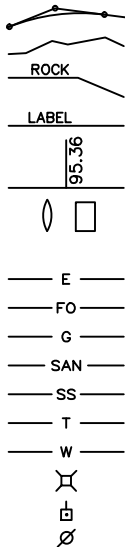
DESIGN DESIGNATION

- A.A.D.T. (2011) = 118,500
- A.A.D.T. = N/A
- D.H.V. = N/A
- D.D. = N/A
- T. = N/A
- DESIGN SPEED = 60 MPH
- ESALS = N/A

CONVENTIONAL SYMBOLS

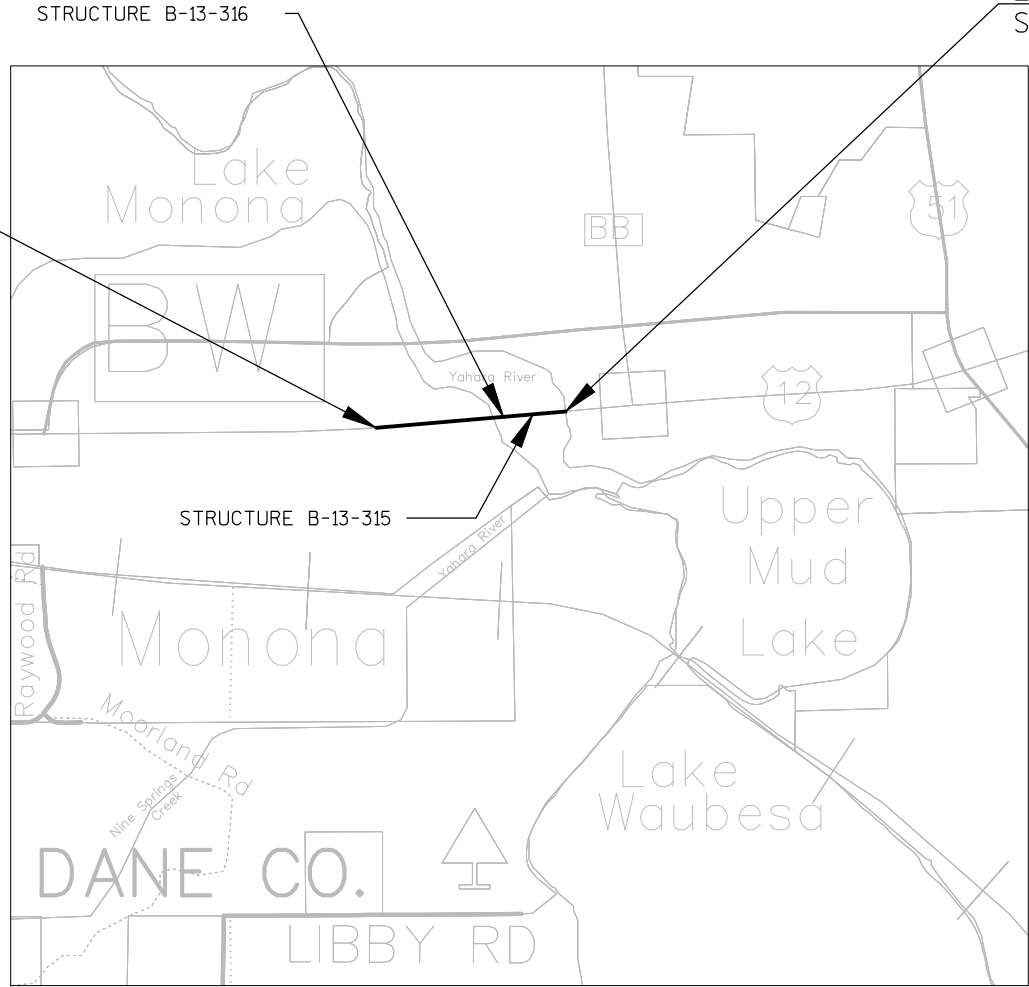
- PLAN
- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS
- MARSH AREA
- WOODED OR SHRUB AREA

- PROFILE
- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such)
- SPECIAL DITCH
- GRADE ELEVATION
- CULVERT (Profile View)
- UTILITIES
- ELECTRIC
- FIBER OPTIC
- GAS
- SANITARY SEWER
- STORM SEWER
- TELEPHONE
- WATER
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE



BEGIN PROJECT
STA. 99+67.57
X = 833,678.452
Y = 471,655.256

STATE PROJECT NUMBER
1206-04-65



LAYOUT
SCALE 0 1/2

TOTAL NET LENGTH OF CENTERLINE = 0.493

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, DANE COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1206-04-65	WISC 2016199	1

ORIGINAL PLANS PREPARED BY

AECOM 342 North Water Street
7th Floor
Milwaukee, WI 53202
(414)831-4100



2/16/16 *WRS*
(Date) (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WISDOT
Designer	AECOM
Project Manager	DAVID LAYTON
Regional Examiner	REGIONAL EXAMINER
Regional Supervisor	BRENDA SCHOENFELD

APPROVED FOR THE DEPARTMENT

DATE: 2/16/2016 *Brenda Schoenfeld*
(Signature)

E

GENERAL NOTES

THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE HIS CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

PROTECT INLETS WITH PROPER INLET PROTECTION AT LOCATIONS EXHIBITING RISK OF BEING IMPACTED BY CONSTRUCTION OPERATIONS AS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY ANY OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS AT THE CONTRACTOR'S EXPENSE.

OTHER AGENCIES

DNR AREA LIAISON

ERIC HEGGELUND
DEPARTMENT OF NATURAL RESOURCES
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
608-275-3301
ERIC.HEGGELUND@WISCONSIN.GOV

WISDOT RWIS (DECK SENSORS)

MICHAEL ADAMS
PROJECT MANAGER
4802 SHEBOYGAN AVENUE, ROOM 501
PO BOX 7986
MADISON, WI 53707-7986
608-266-5004
MICHAEL.ADAMS@DOT.WI.GOV

DESIGN CONTACTS

DAVID LAYTON
WISDOT - SW REGION
2101 WRIGHT STREET
MADISON, WI 53704
608-246-3821
DAVID.LAYTON@DOT.WI.GOV

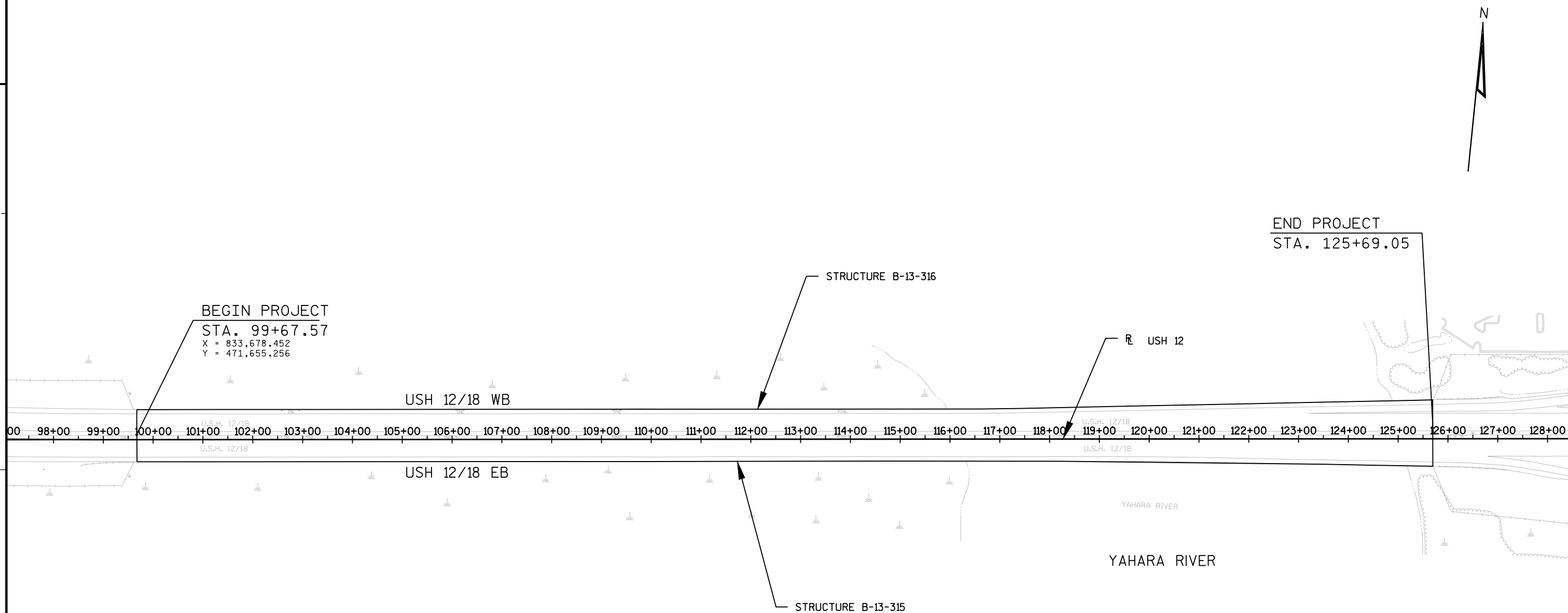
WILLIAM SCHILLING
AECOM
1555 N RIVERCENTER DRIVE, STE 214
MILWAUKEE, WI 53212
(414) 944-6201
BILL.SCHILLING@AECOM.COM

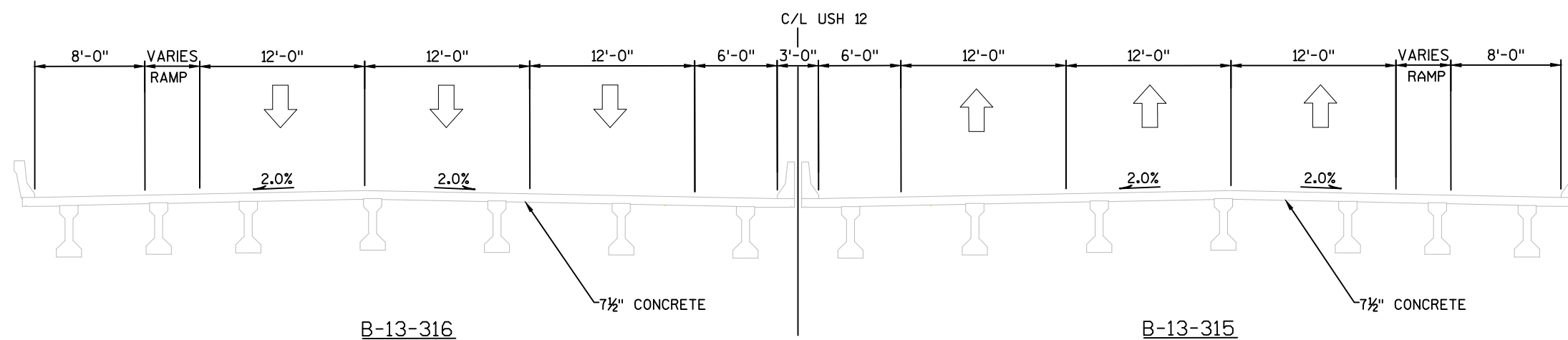
ABBREVIATIONS

AEW	APRON END WALL
AGG	AGGREGATE
BAD	BASE AGGREGATE DENSE
BM	BENCH MARK
C&G	CURB AND GUTTER
C/L	CENTER OR CONSTRUCTION LINE
CMCP	CULVERT PIPE CORRUGATED METAL
CONC	CONCRETE
CP	CULVERT PIPE
CPRC	CULVERT PIPE REINFORCED CONCRETE
CSD	CONCRETE SURFACE DRAIN
CY	CUBIC-YARD
D	DEGREE OF CURVE
Δ	DELTA
DISCH	DISCHARGE
EAT	ENERGY ABSORBING TERMINAL
FE	FIELD ENTRANCE
HMA	HOT MIX ASPHALT
INV	INVERT
L	LENGTH OF CURVE
LHF	LEFT HAND FORWARD
LT	LEFT
MIN	MINIMUM
ML	MATCHLINE
NB	NORTHBOUND
NC	NORMAL CROWN
NIC	NOT IN CONTRACT
PAVT	PAVEMENT
PC	POINT OF CURVE
PCC	POINT OF COMPOUND CURVE
PE	PRIVATE ENTRANCE
PI	POINT OF INTERSECTION
PLE	PERMANENT LIMITED EASEMENT
PT	POINT OF TANGENT
R	RADIUS OF CURVE
R/L	REFERENCE LINE
R/W	RIGHT OF WAY
RC	REVERSE CROWN
RCAEW	APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE
REQD	REQUIRED
RHF	RIGHT HAND FORWARD
RO	RUN OFF LENGTH
RRSP	RAILROAD SPIKE
RT	RIGHT
SALV	SALVAGED
SAPBC	SALVAGED ASPHALTIC PAVEMENT BASE COARSE
SB	SOUTHBOUND
SDD	STANDARD DETAIL DRAWINGS
SE	SUPER ELEVATION
SF	SQUARE FOOT
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
STA	STATION
SY	SQUARE YARD
T	TANGENT LENGTH
TLE	TEMPORARY LIMITED EASEMENT
VCL	VERTICAL CURVE LENGTH
VPC	POINT OF VERTICAL CURVE
VPI	POINT OF VERTICAL INTERSECTION
VPT	POINT OF VERTICAL TANGENT

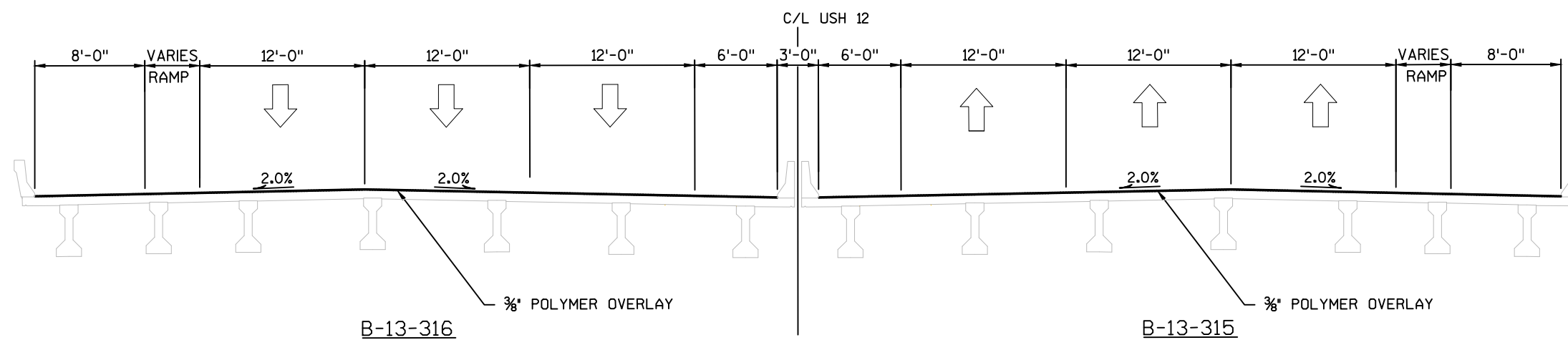


Dial 811 or (800) 242-8511
www.DiggersHotline.com

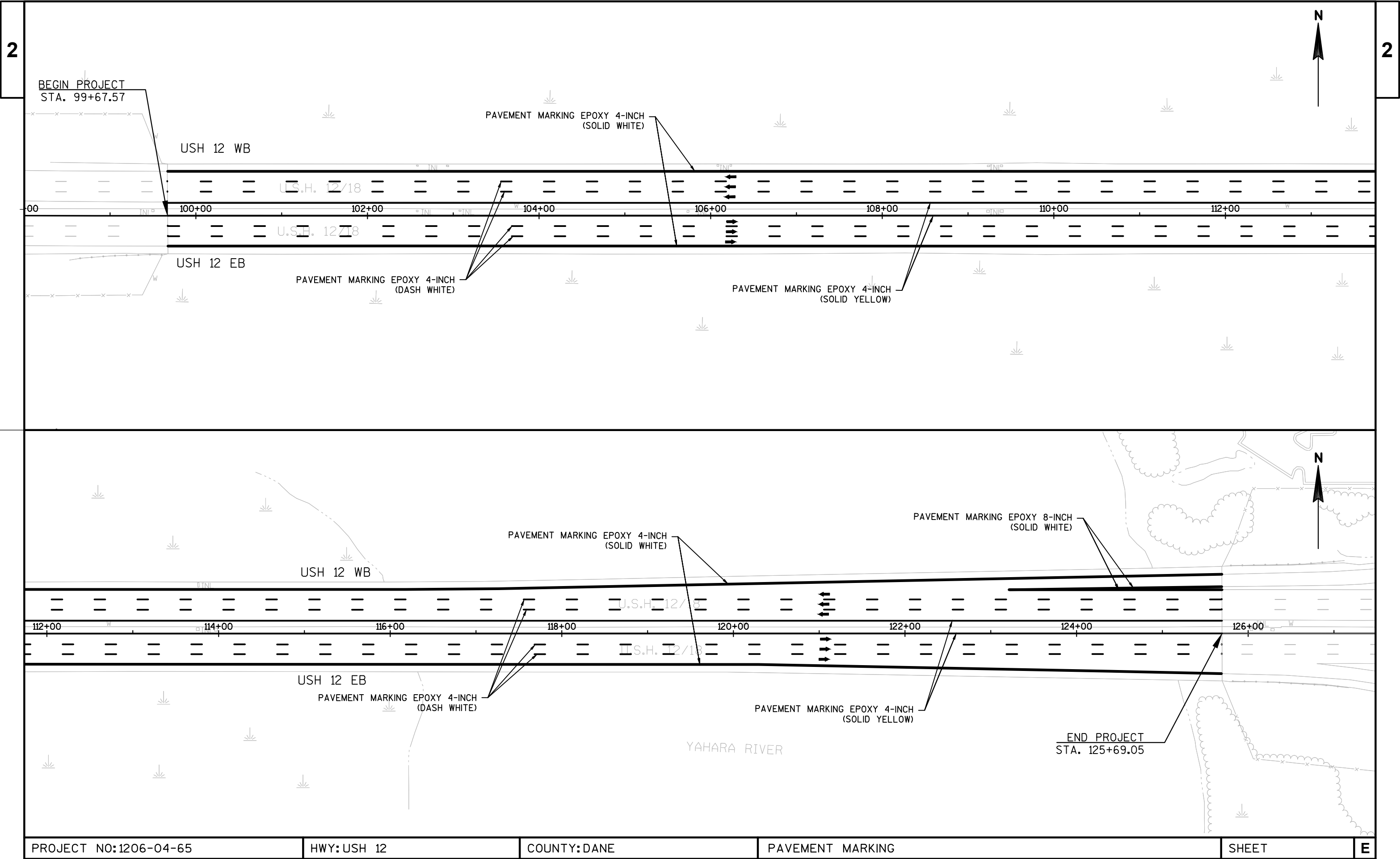




TYPICAL EXISTING SECTION
STA 99+67.57 - STA 125+69.05



TYPICAL FINISHED SECTION
STA 99+67.57 - STA 125+69.05



GENERAL TRAFFIC CONTROL NOTES

THE EXACT LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM OF 1,500 FEET IN FRONT OF DRUMS.

THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS. USE SUPPORTS THAT PROVIDE A MINIMUM OF 5 FEET FROM THE BOTTOM OF THE SIGN TO THE PAVEMENT.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO LOCATE THE ARROWBOARD SHOWN, PLACE THE ARROWBOARD IN THE LANE CLOSURE TAPER AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE TAPER.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF THE WORK DAY.

TRAFFIC CONTROL DRUMS IN TAPERS SHALL BE EQUIPPED WITH WARNING LIGHTS TYPE "C" ONE-WAY LIGHTS IN TAPERS ONLY UNLESS OTHERWISE SHOWN.

CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING.

ADJUSTMENTS IN THE BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

MAINTAIN ALL LANES OF USH 12 EASTBOUND AND WESTBOUND TRAFFIC DURING PEAK HOURS.













SEE SDD 'TRAFFIC CONTROL LANE CLOSURES SPEEDS GREATER THAN 40 MPH', 'TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY (SHORT TERM)', 'EXIT RAMP CLOSURE', AND 'SHOULDER CLOSURE ON DIVIDED ROADWAY SPEEDS GREATER THAN 40 MPH' FOR SHOULDER AND LANE CLOSURE INFORMATION.

STAGING NOTES

TRAFFIC CONTROL NOTES FOR EACH SPECIFIC STAGE WILL BE PROVIDED AT THE BEGINNING OF THE STAGING PLANS.

SEE SPECIAL PROVISIONS FOR TRAFFIC STAGING REQUIREMENTS.

LEGEND

-  TYPE III BARRICADE
-  TYPE III BARRICADE WTH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  FLASHING ARROW BOARD
-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  TYPE C WARNING LIGHT (STEADY BURN)
-  WORK AREA
-  DIRECTION OF TRAFFIC
-  PORTABLE CHANGEABLE MESSAGE BOARD
-  POLYMER OVERLAY AREA

CONSTRUCTION STAGING NOTES

STAGE 1 TRAFFIC CONTROL NOTES

- 1. CLOSE INSIDE SHOULDER AND ADJACENT LANE (LANE 1)
- 2. OVERLAY INSIDE SHOULDER OF B-13-315

STAGE 2 TRAFFIC CONTROL NOTES

- 1. CLOSE INSIDE SHOULDER AND THE TWO (2) INNERMOST LANES (LANE 1 & 2)
- 2. OVERLAY INNERMOST EASTBOUND LANE (LANE 1)

STAGE 3 TRAFFIC CONTROL NOTES

- 1. CLOSE INSIDE SHOULDER AND THE TWO (2) INNERMOST LANES (LANE 1 & 2)
- 2. OUTSIDE LANE (LANE 3) WILL BE NARROWED AND SHIFTED. TRAFFIC WILL USE LANE 3 AND OUTSIDE SHOULDER AS SINGLE DRIVING LANE.
- 3. OVERLAY MIDDLE LANE (LANE 2)

STAGE 4 TRAFFIC CONTROL NOTES

- 1. CLOSE SHOULDER, RAMP LANE, AND THE TWO (2) OUTERMOST LANES (LANE 2 & 3)
- 2. OVERLAY OUTSIDE LANE (LANE 3)

STAGE 5 TRAFFIC CONTROL NOTES

- 1. CLOSE OUTSIDE LANE (LANE 3), RAMP LANE, AND OUTSIDE SHOULDER
- 2. OVERLAY OUTSIDE SHOULDER AND RAMP LANE

STAGE 6 TRAFFIC CONTROL NOTES

- 1. CLOSE INSIDE SHOULDER AND ADJACENT LANE (LANE 1)
- 2. OVERLAY INSIDE SHOULDER OF B-13-316

STAGE 7 TRAFFIC CONTROL NOTES

- 1. CLOSE INSIDE SHOULDER AND THE TWO (2) INNERMOST LANES (LANE 1 & 2)
- 2. OVERLAY INNERMOST WESTBOUND LANE (LANE 1)

STAGE 8 TRAFFIC CONTROL NOTES

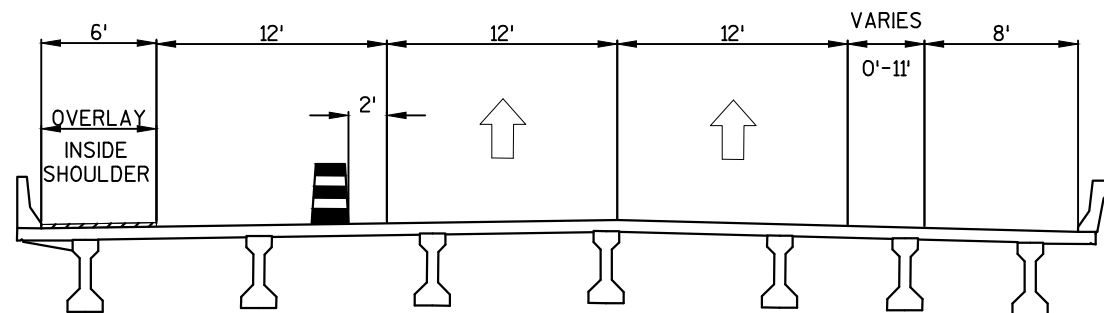
- 1. CLOSE INSIDE SHOULDER AND THE TWO (2) INNERMOST LANES (LANE 1 & 2)
- 2. OUTSIDE LANE (LANE 3) WILL BE NARROWED AND SHIFTED. TRAFFIC WILL USE LANE 3 AND OUTSIDE SHOULDER AS SINGLE DRIVING LANE.
- 3. OVERLAY MIDDLE LANE (LANE 2)

STAGE 9 TRAFFIC CONTROL NOTES

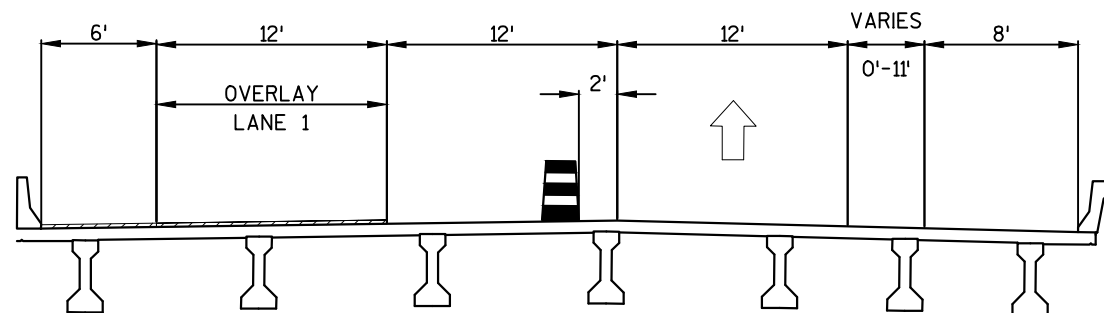
- 1. CLOSE SHOULDER, RAMP LANE, AND THE TWO (2) OUTERMOST LANES (LANE 2 & 3)
- 2. OVERLAY OUTSIDE LANE (LANE 3)

STAGE 10 TRAFFIC CONTROL NOTES

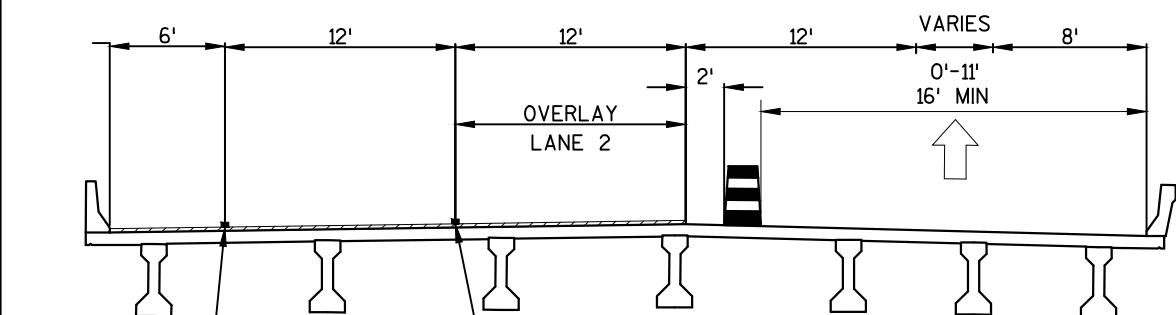
- 1. CLOSE OUTSIDE LANE (LANE 3), RAMP LANE, AND OUTSIDE SHOULDER
- 2. OVERLAY OUTSIDE SHOULDER AND RAMP LANE



STAGE 1 - NIGHTTIME WORK
OVERLAY ENTIRE INSIDE SHOULDER
B-13-315



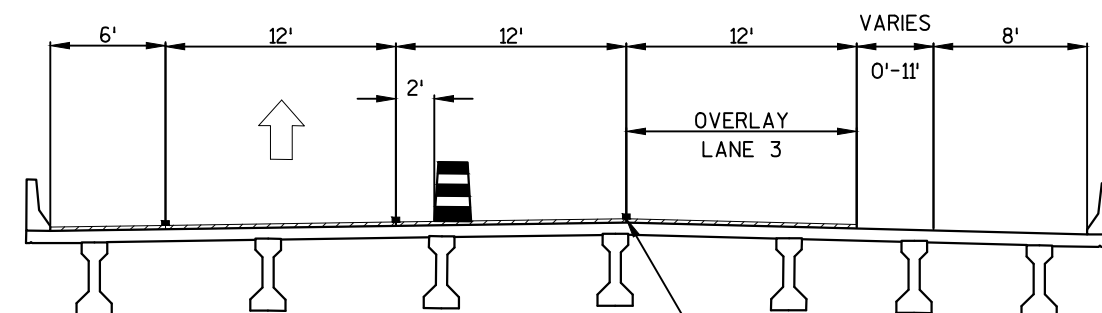
STAGE 2 - NIGHTTIME WORK
OVERLAY LANE 1
B-13-315



PAVEMENT MARKING EPOXY,
4-INCH
(SOLID YELLOW)

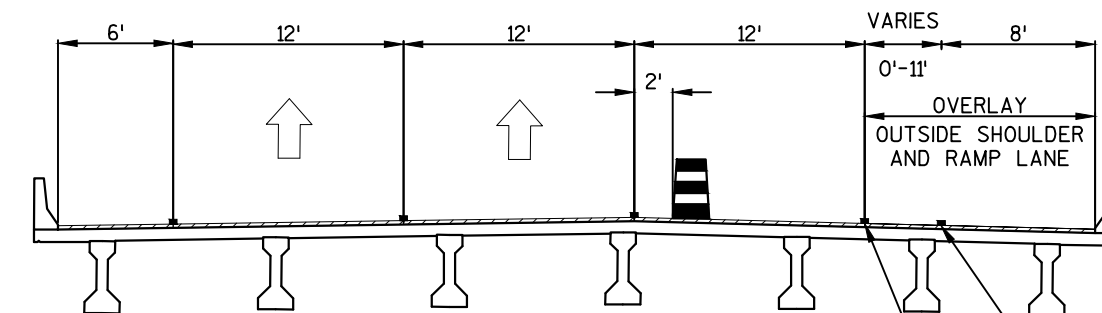
STAGE 3 - NIGHTTIME WORK
OVERLAY LANE 2
B-13-315

PAVEMENT MARKING EPOXY,
4-INCH (DASHED WHITE)



STAGE 4 - NIGHTTIME WORK
OVERLAY LANE 3
B-13-315

PAVEMENT MARKING EPOXY,
4-INCH (DASHED WHITE)

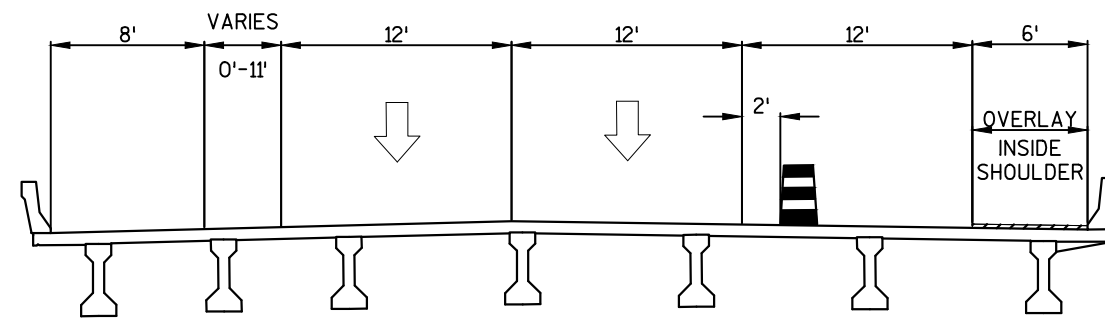


STAGE 5 - NIGHTTIME WORK
OVERLAY OUTSIDE SHOULDER AND RAMP LANE
B-13-315

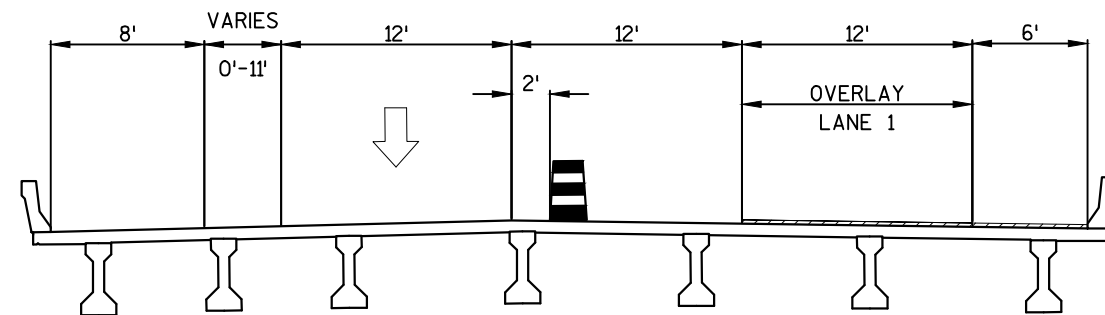
PAVEMENT MARKING EPOXY,
4-INCH
(SOLID WHITE)

PAVEMENT MARKING EPOXY,
8-INCH
(SOLID WHITE) AT GORE

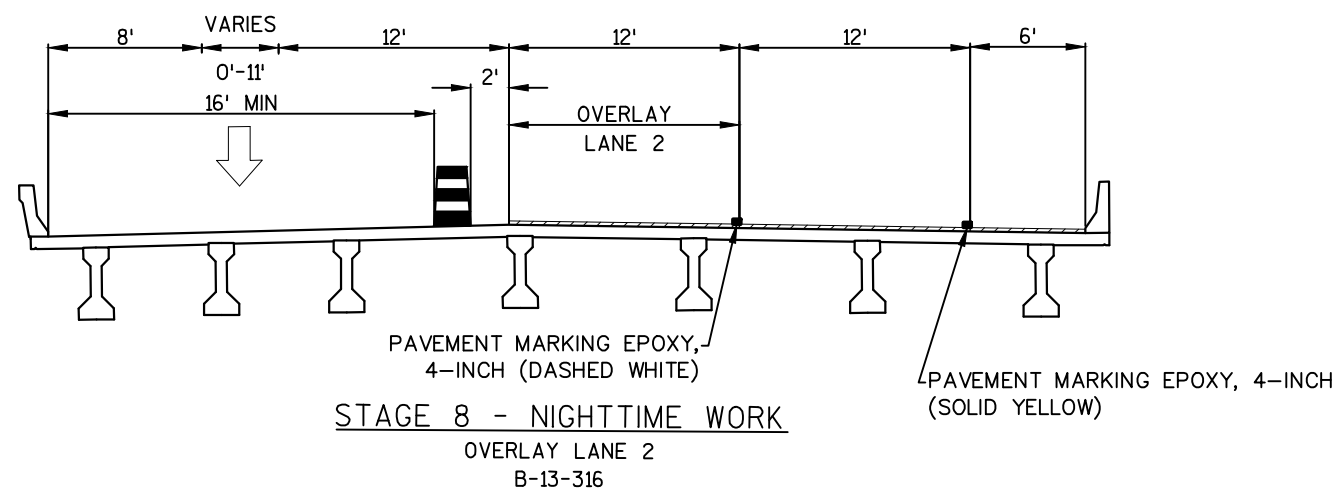
NOTE:
PLACE DASHED WHITE LANE LINE MARKINGS THE FIRST NIGHT OF THE STAGE INDICATED PRIOR TO REOPENING LANES TO TRAFFIC.



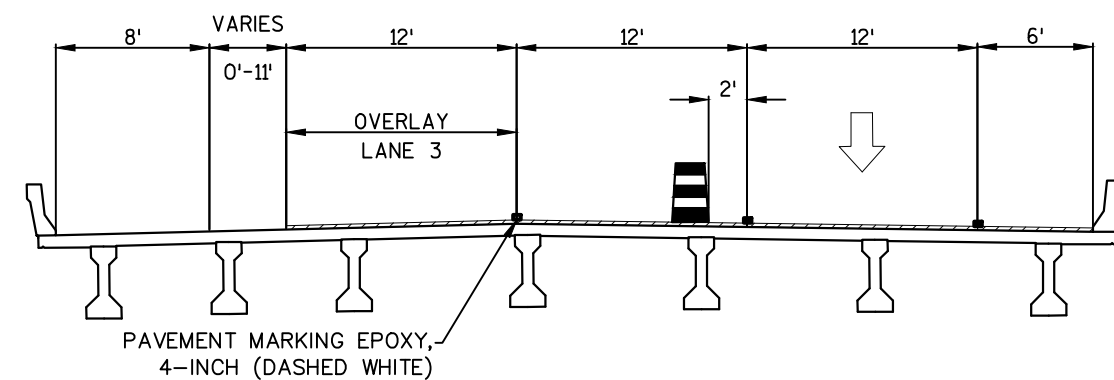
STAGE 6 - NIGHTTIME WORK
OVERLAY ENTIRE INSIDE SHOULDER
B-13-316



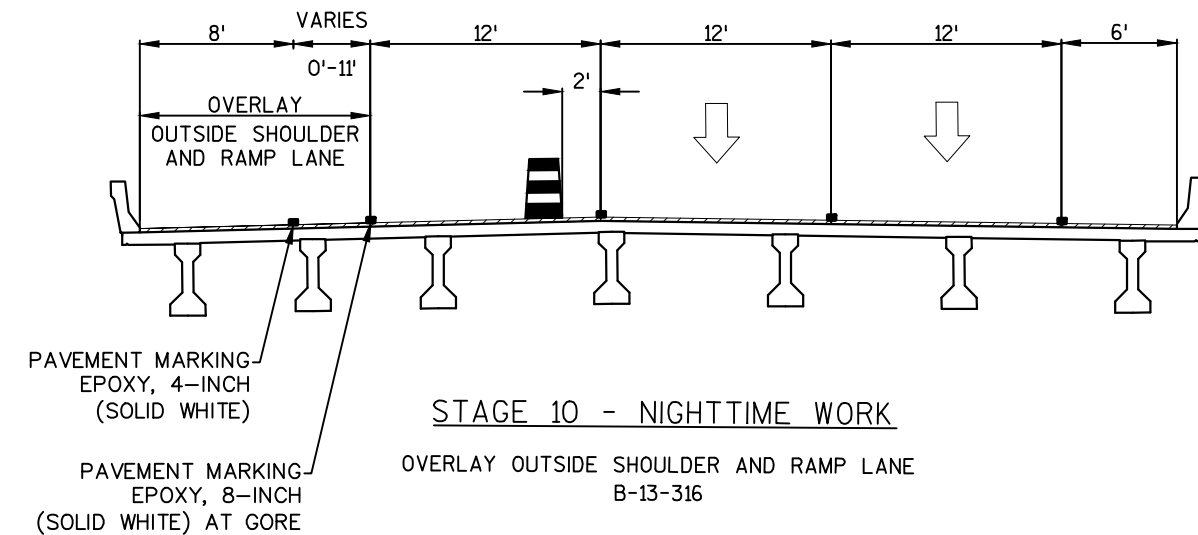
STAGE 7 - NIGHTTIME WORK
OVERLAY LANE 1
B-13-316



STAGE 8 - NIGHTTIME WORK
OVERLAY LANE 2
B-13-316

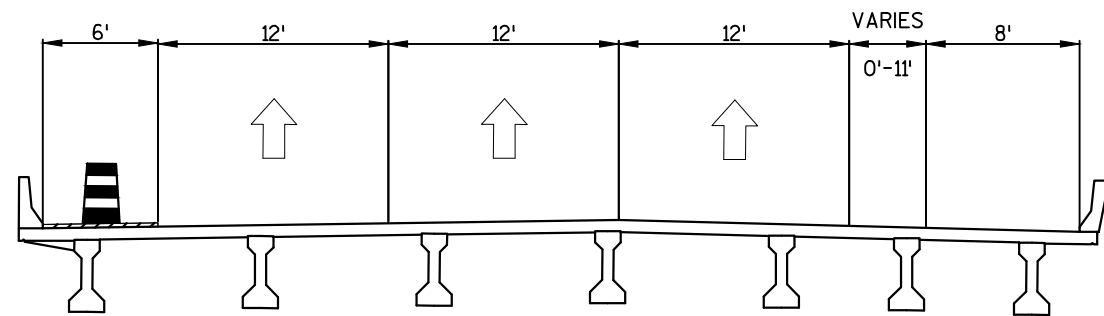


STAGE 9 - NIGHTTIME WORK
OVERLAY LANE 3
B-13-316

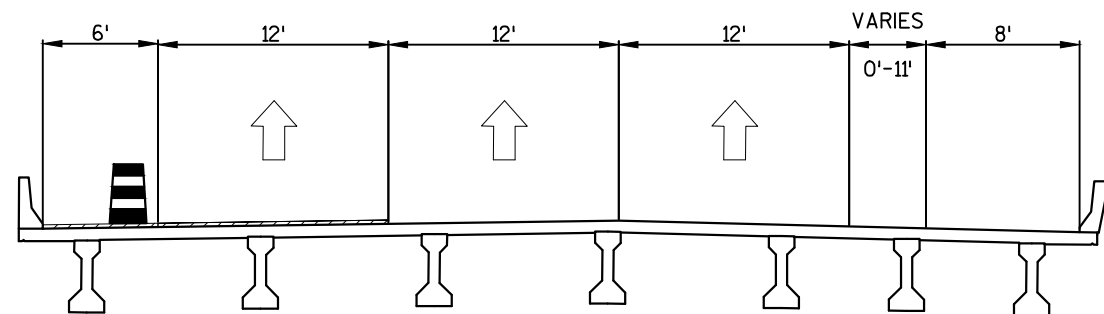


STAGE 10 - NIGHTTIME WORK
OVERLAY OUTSIDE SHOULDER AND RAMP LANE
B-13-316

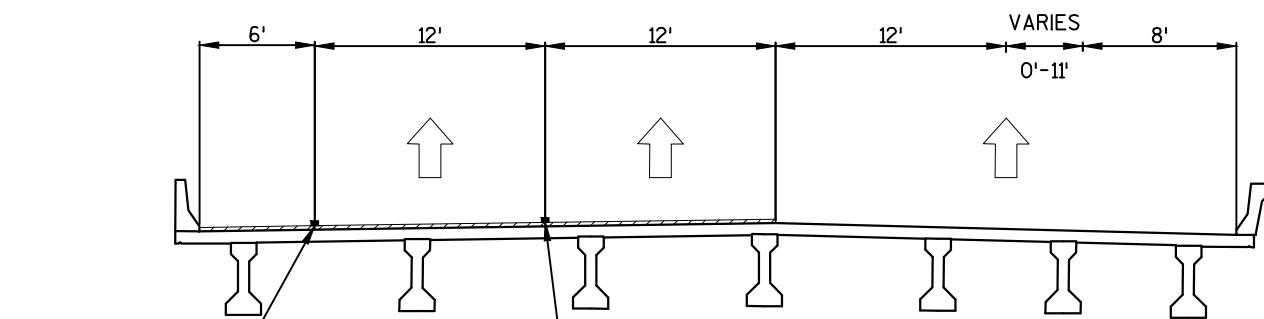
NOTE:
PLACE DASHED WHITE LANE LINE MARKINGS THE FIRST NIGHT OF THE STAGE INDICATED
PRIOR TO REOPENING LANES TO TRAFFIC



STAGE 1 - DAYTIME
OVERLAY ENTIRE INSIDE SHOULDER
B-13-315



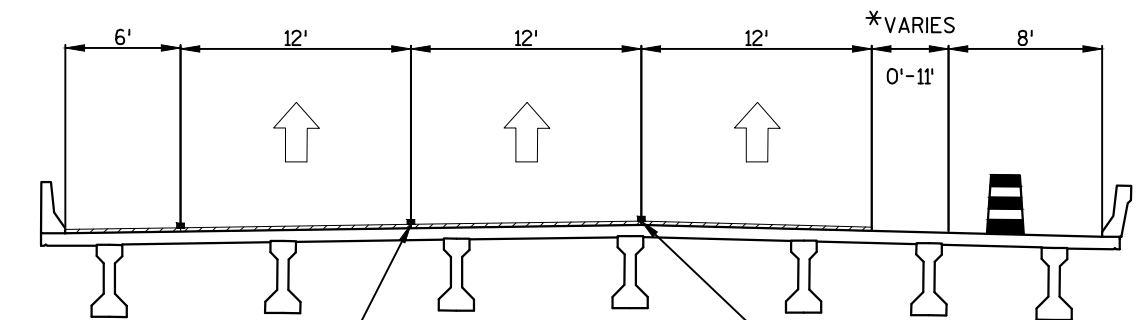
STAGE 2 - DAYTIME
OVERLAY LANE 1
B-13-315



PAVEMENT MARKING
EPOXY, 4-INCH
(SOLID YELLOW)

PAVEMENT MARKING EPOXY,
4-INCH (DASHED WHITE)

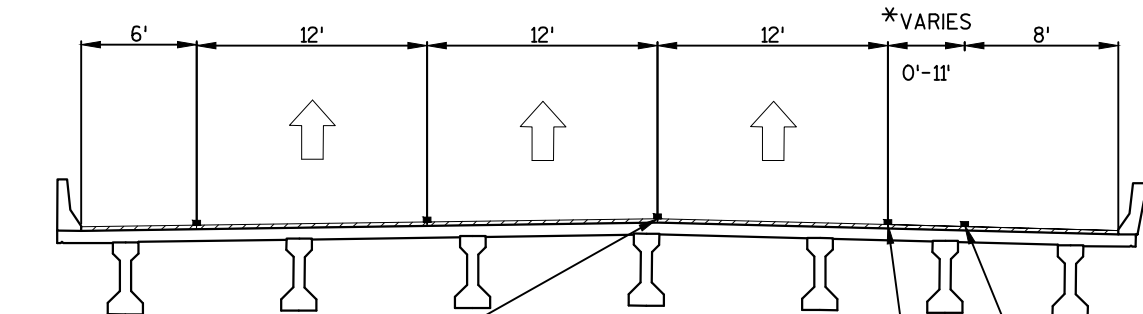
STAGE 3 - DAYTIME
OVERLAY LANE 2
B-13-315



PAVEMENT MARKING EPOXY,
4-INCH (DASHED WHITE)

STAGE 4 - DAYTIME
OVERLAY LANE 3
B-13-315

PAVEMENT MARKING EPOXY,
4-INCH (DASHED WHITE)



PAVEMENT MARKING EPOXY, 4-INCH
(DASHED WHITE)

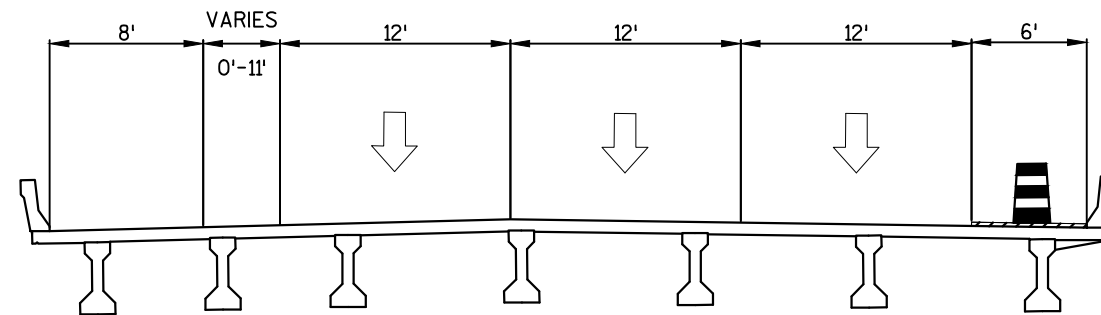
STAGE 5 - DAYTIME
OVERLAY OUTSIDE SHOULDER AND RAMP LANE
B-13-315

PAVEMENT MARKING EPOXY, 4-INCH
(SOLID WHITE)

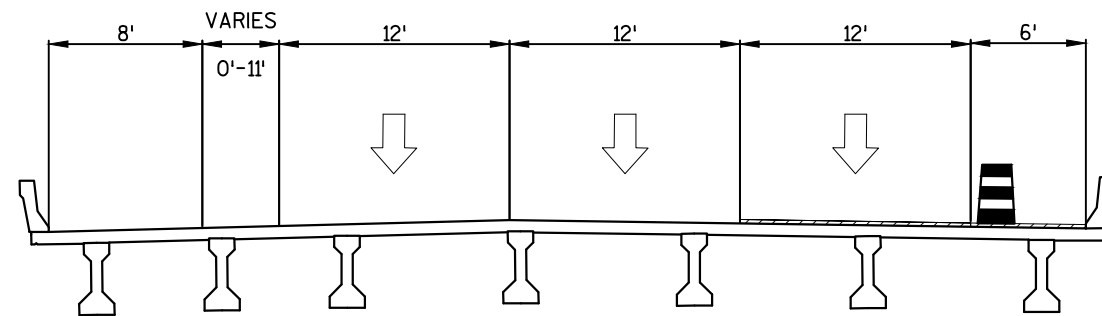
PAVEMENT MARKING EPOXY, 8-INCH
(SOLID WHITE) AT GORE

NOTE:
PLACE PAVEMENT MARKINGS AT THE END OF THE NIGHT PRIOR TO OPENING LANES
TO TRAFFIC AND AFTER OVERLAY HAS CURED.

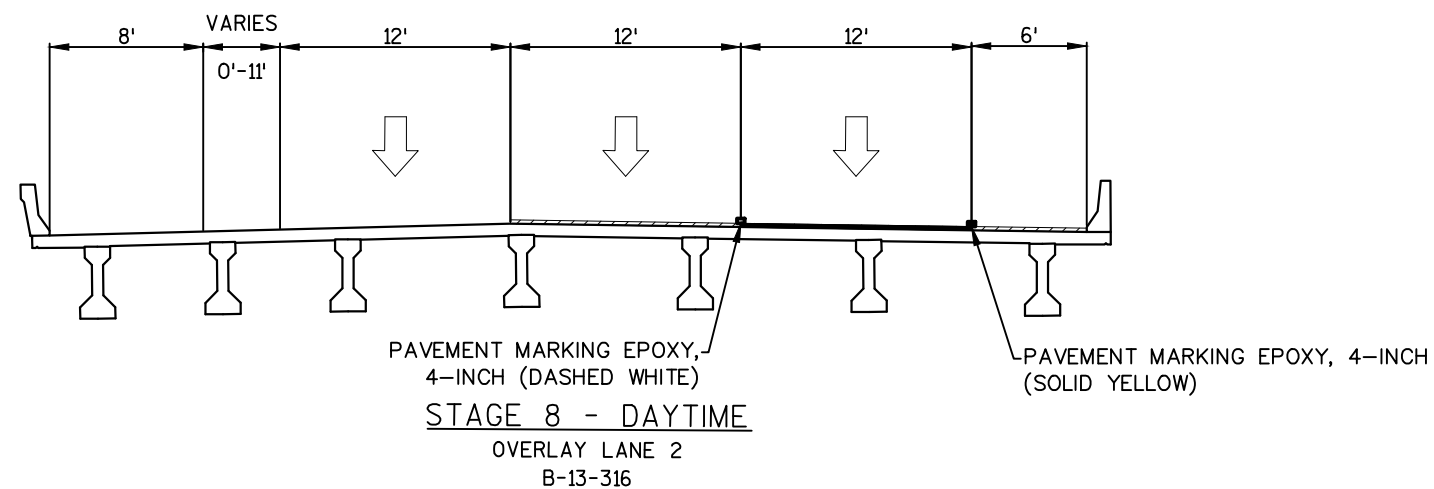
*PLACE DRUMS IN GORE TAPERS TO CLEARLY DIRECT TRAFFIC.
VERIFY WITH FIELD ENGINEER FOR USE AND PLACEMENT.



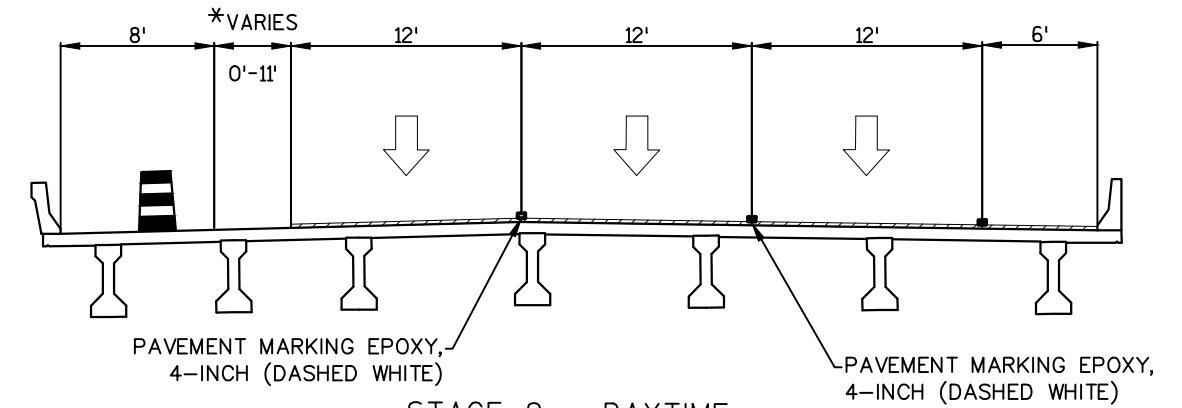
STAGE 6 - DAYTIME
OVERLAY ENTIRE INSIDE SHOULDER
B-13-316



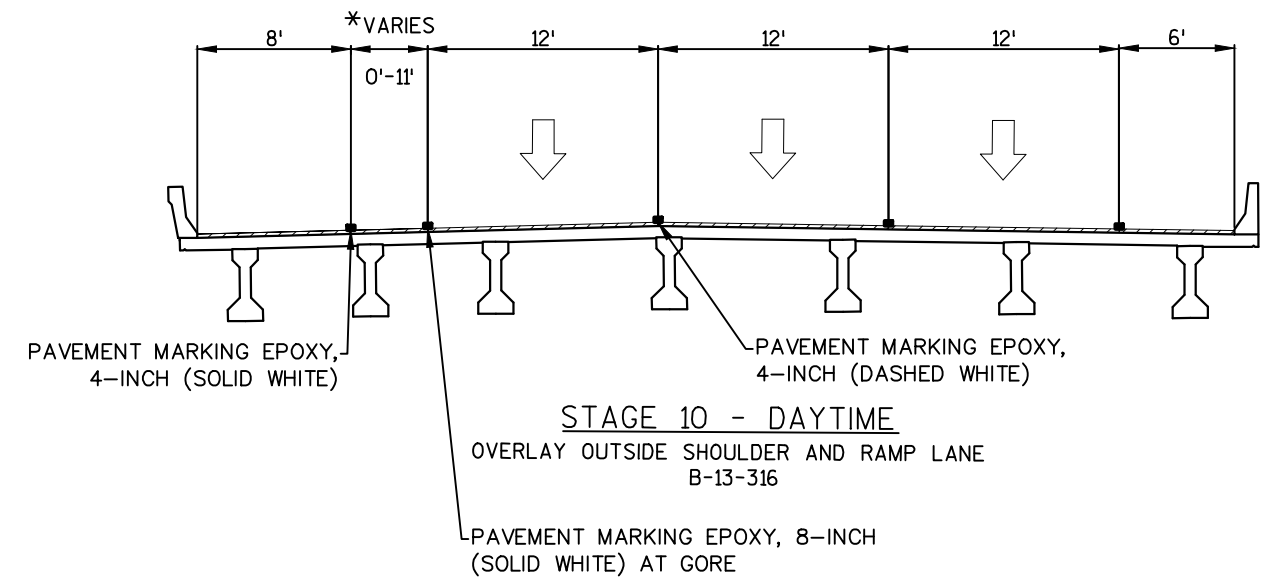
STAGE 7 - DAYTIME
OVERLAY LANE 1
B-13-316



STAGE 8 - DAYTIME
OVERLAY LANE 2
B-13-316



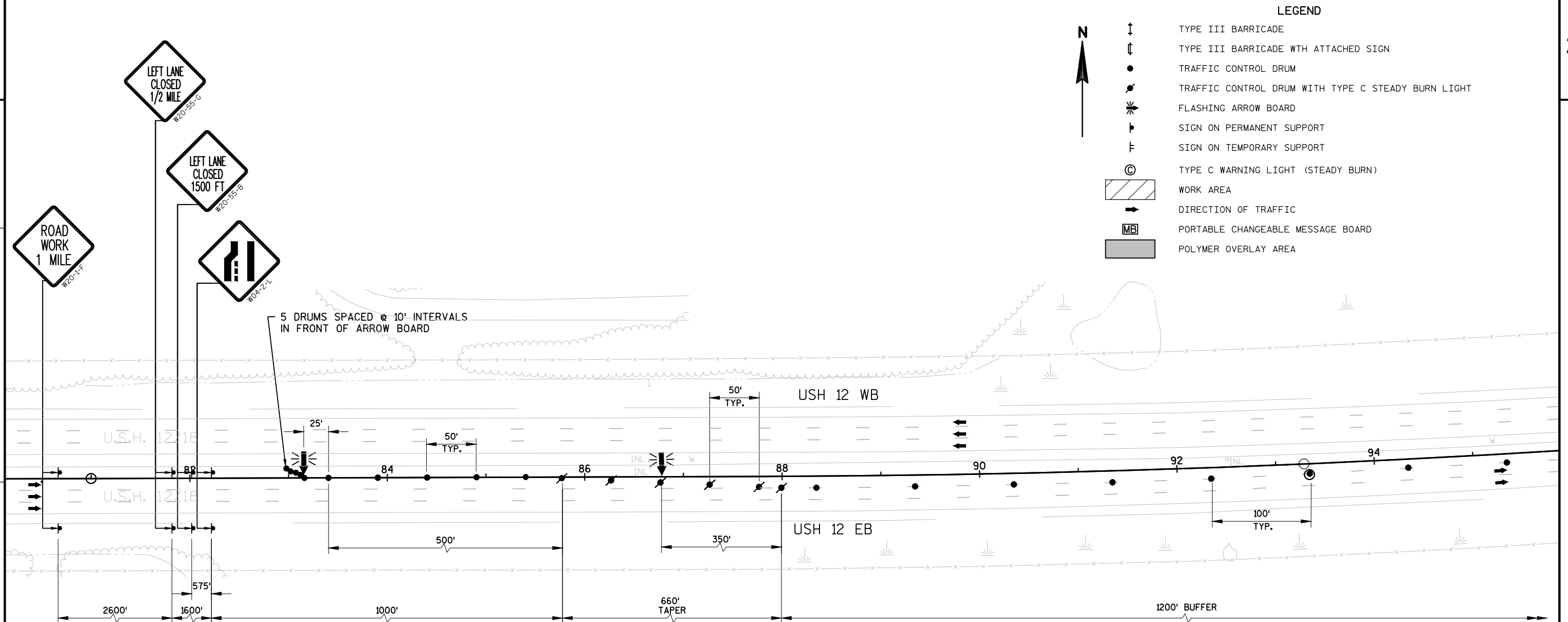
STAGE 9 - DAYTIME
OVERLAY LANE 3
B-13-316

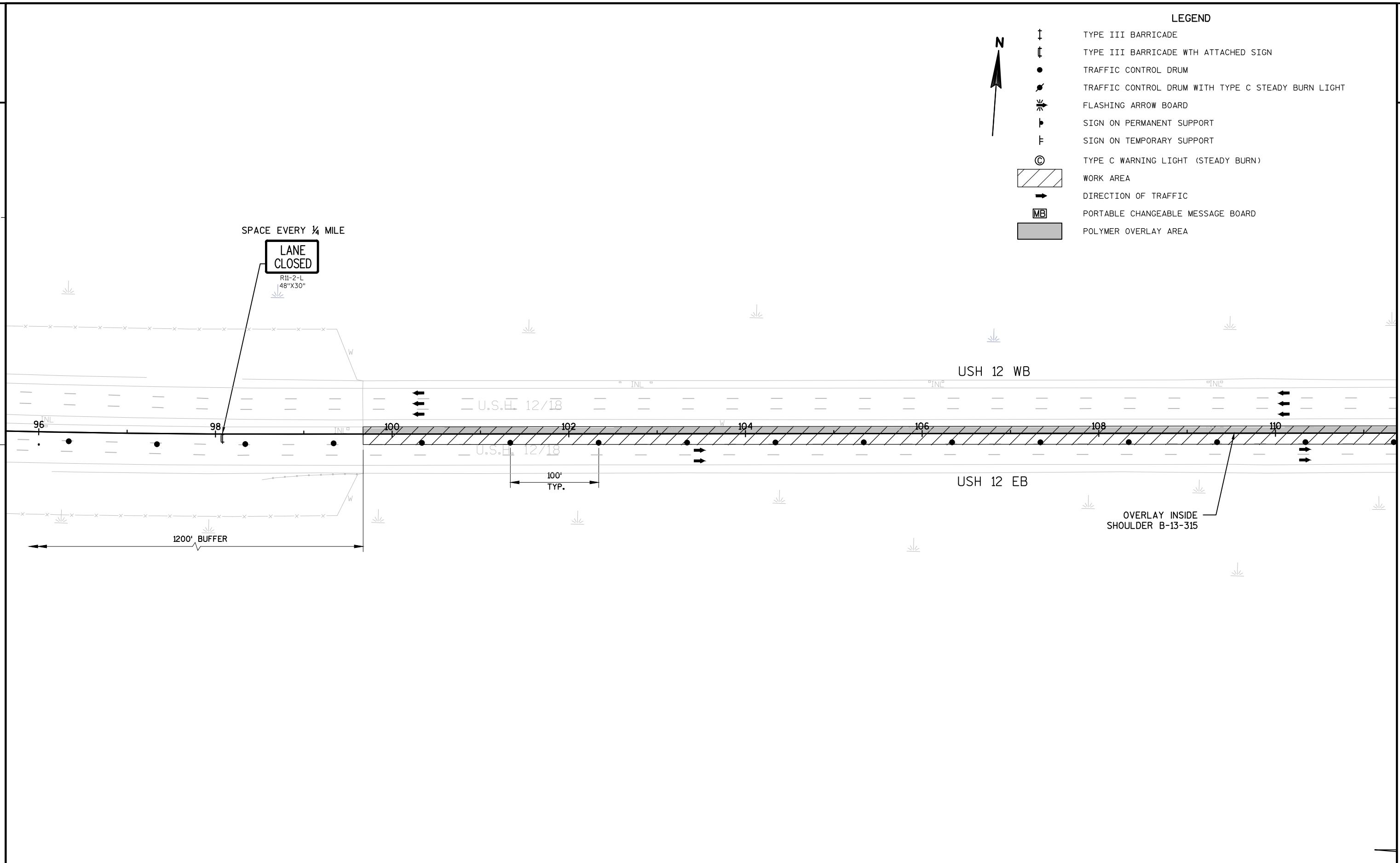


STAGE 10 - DAYTIME
OVERLAY OUTSIDE SHOULDER AND RAMP LANE
B-13-316

NOTE:
PLACE PERMANENT MARKINGS AT THE END OF THE NIGHT PRIOR TO OPENING LANES TO TRAFFIC AND AFTER THE OVERLAY HAS CURED.













*PLACE DRUMS IN GORE TAPERS TO CLEARLY DIRECT TRAFFIC.
VERIFY WITH FIELD ENGINEER FOR USE AND PLACEMENT

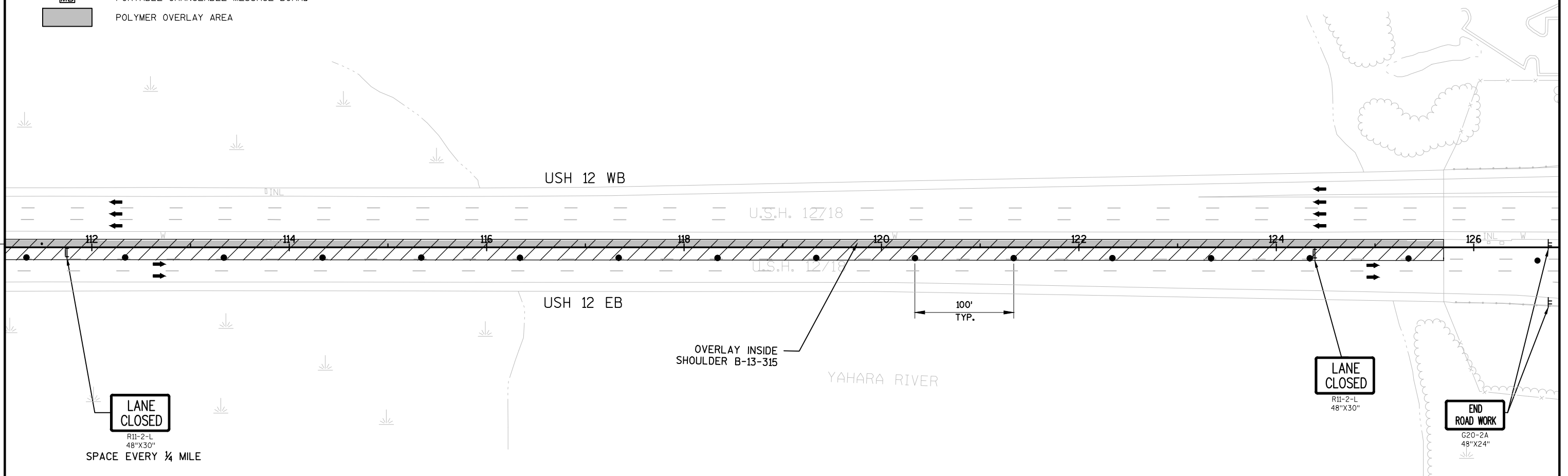


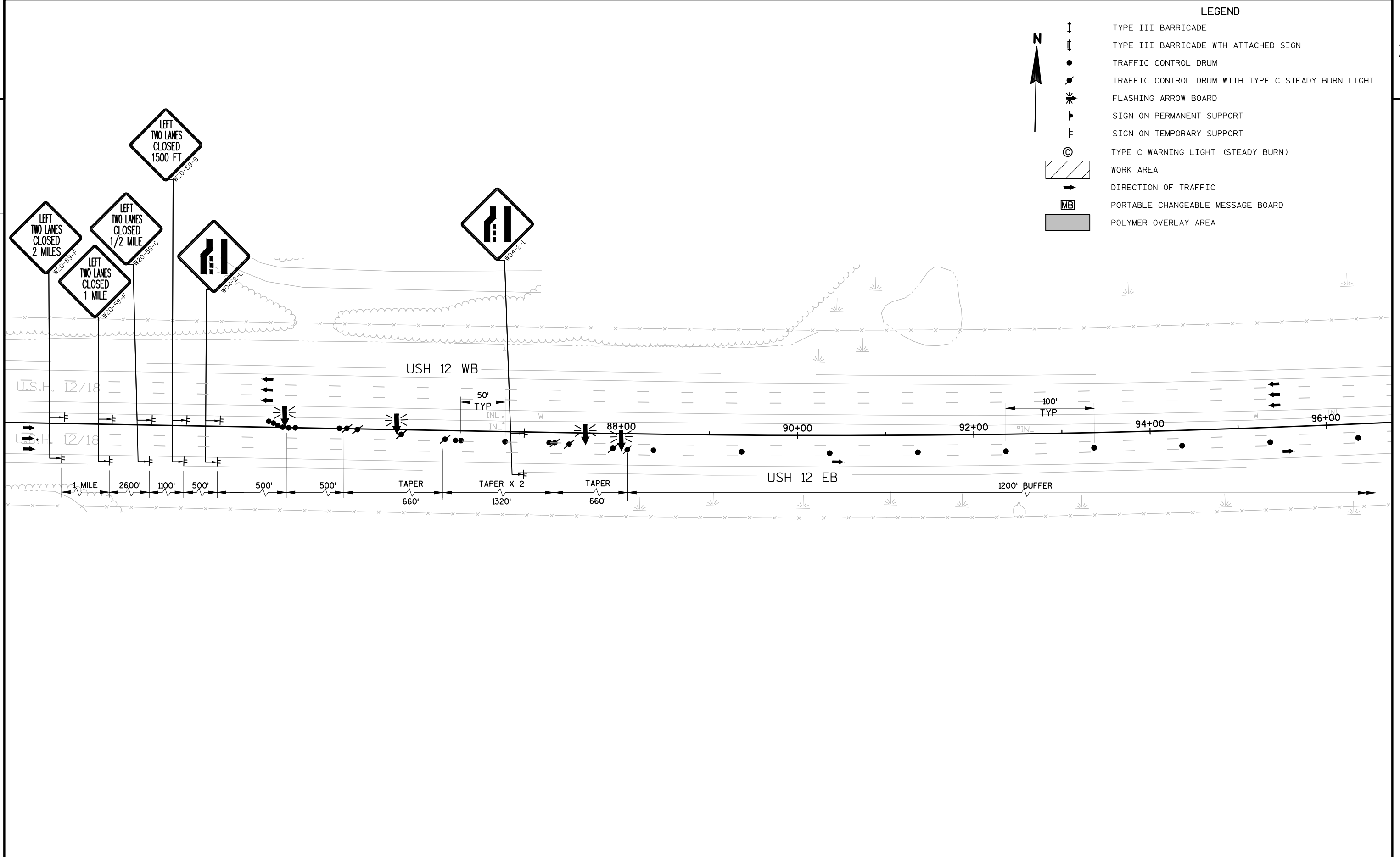


PROJECT NO:1206-04-65	HWY:USH 12	COUNTY:DANE	TRAFFIC CONTROL STAGE 1	SHEET	E
-----------------------	------------	-------------	-------------------------	-------	---

LEGEND

	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	FLASHING ARROW BOARD
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	TYPE C WARNING LIGHT (STEADY BURN)
	WORK AREA
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD
	POLYMER OVERLAY AREA





PROJECT NO:1206-04-65

HWY:USH 12

COUNTY:DANE

TRAFFIC CONTROL STAGE 2

SHEET

E

FILE NAME : P:\TRANSPORTATION\YAHARA BRIDGE\12060433\SHEETS PLAN\025200_TC.DWG
LAYOUT NAME - 025201_TC

PLOT DATE : 2/16/2016 10:00 AM

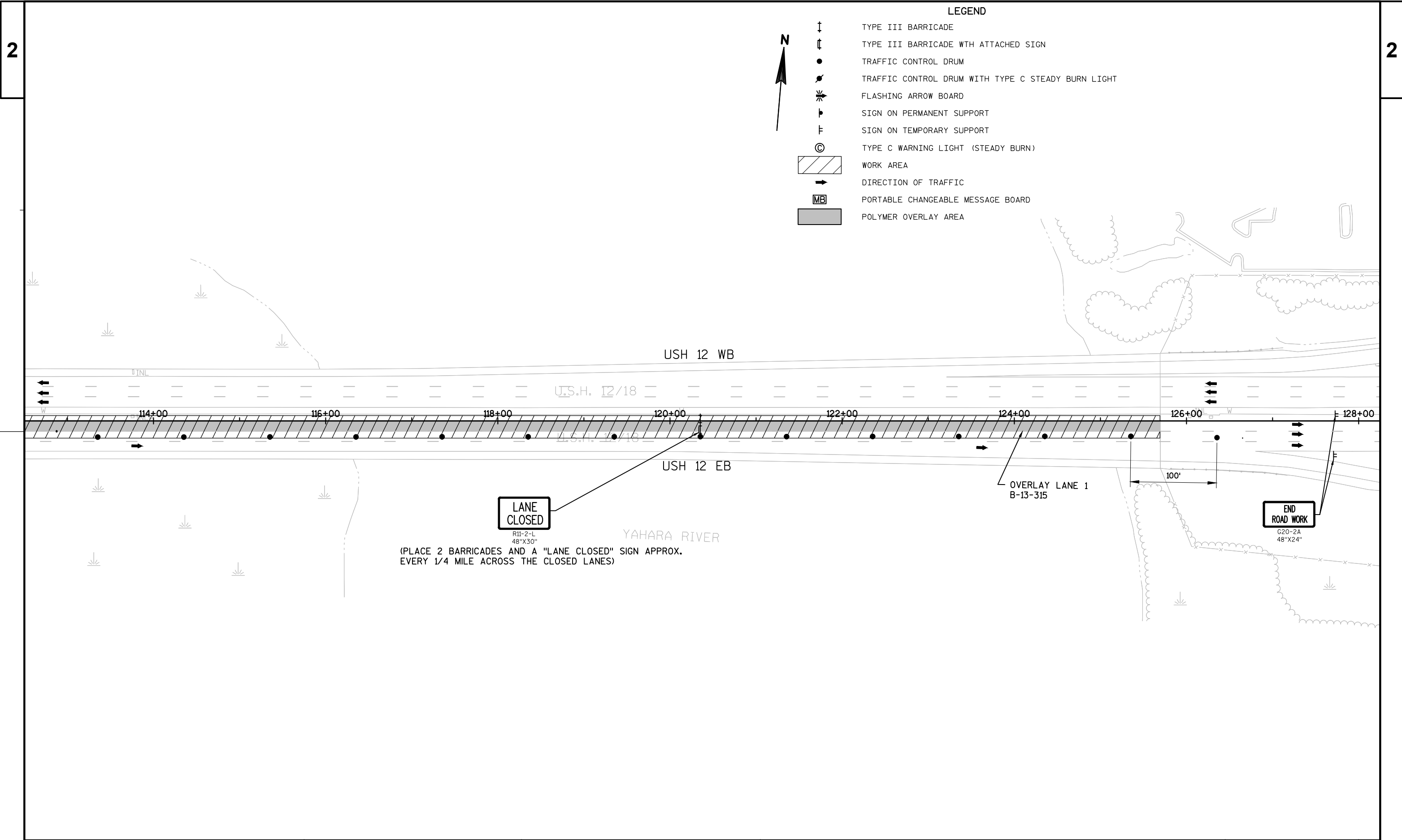
PLOT BY : LANCELLE, JESSICA

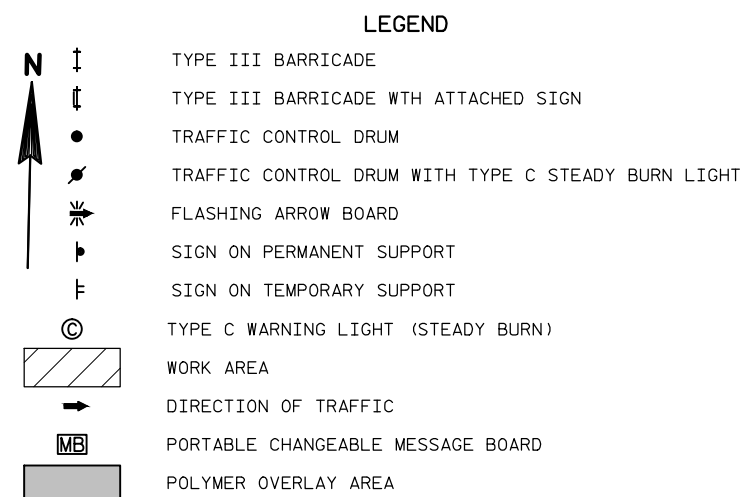
PLOT NAME :

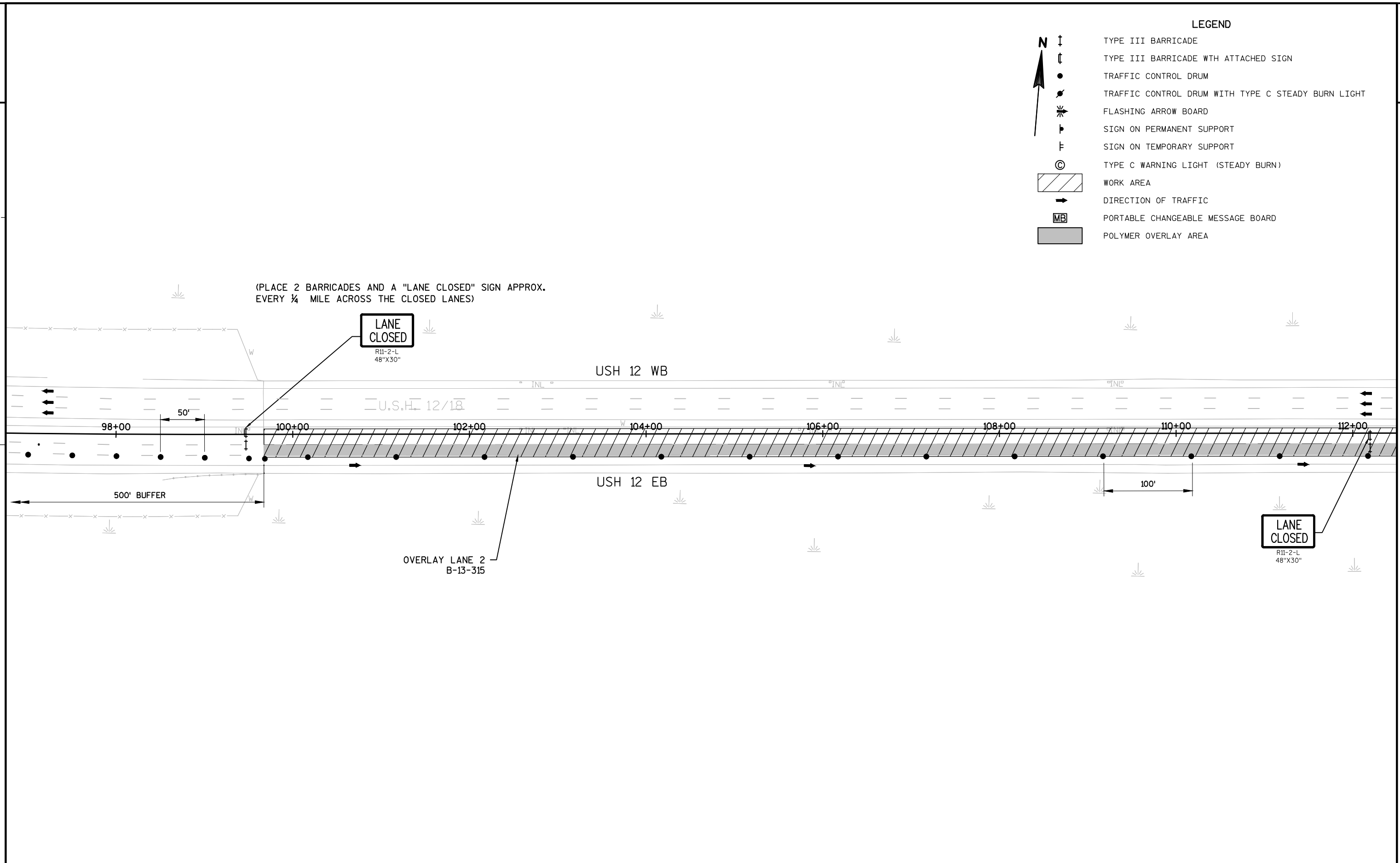
PLOT SCALE : 1:100_XREF

WISDOT/CADDs SHEET 42



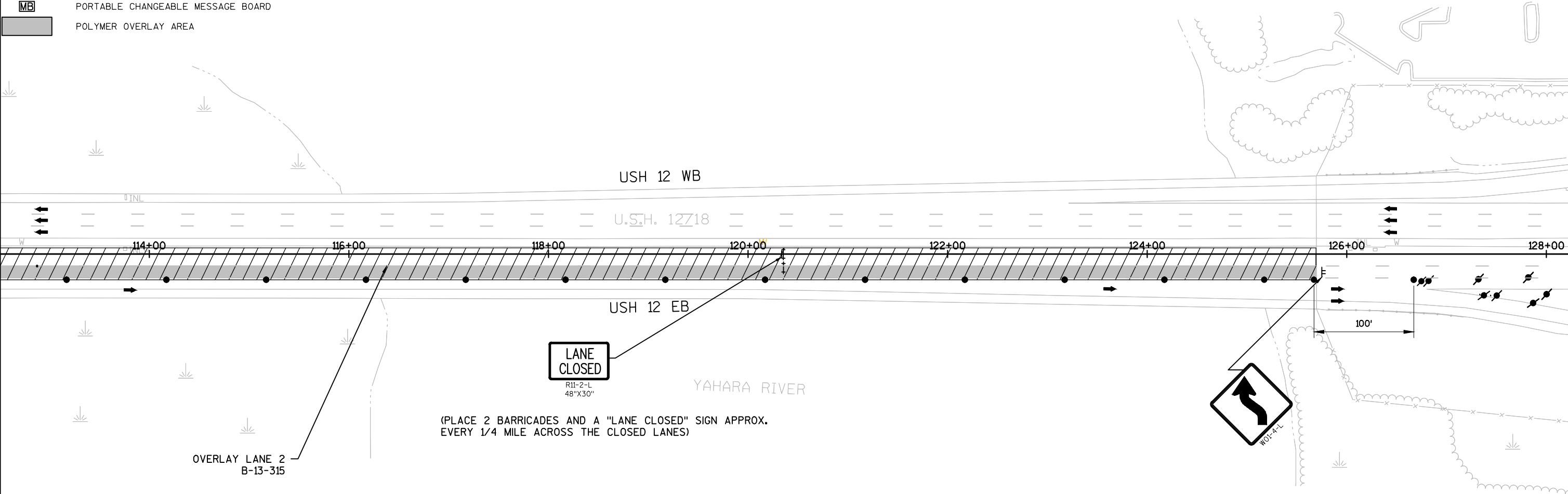


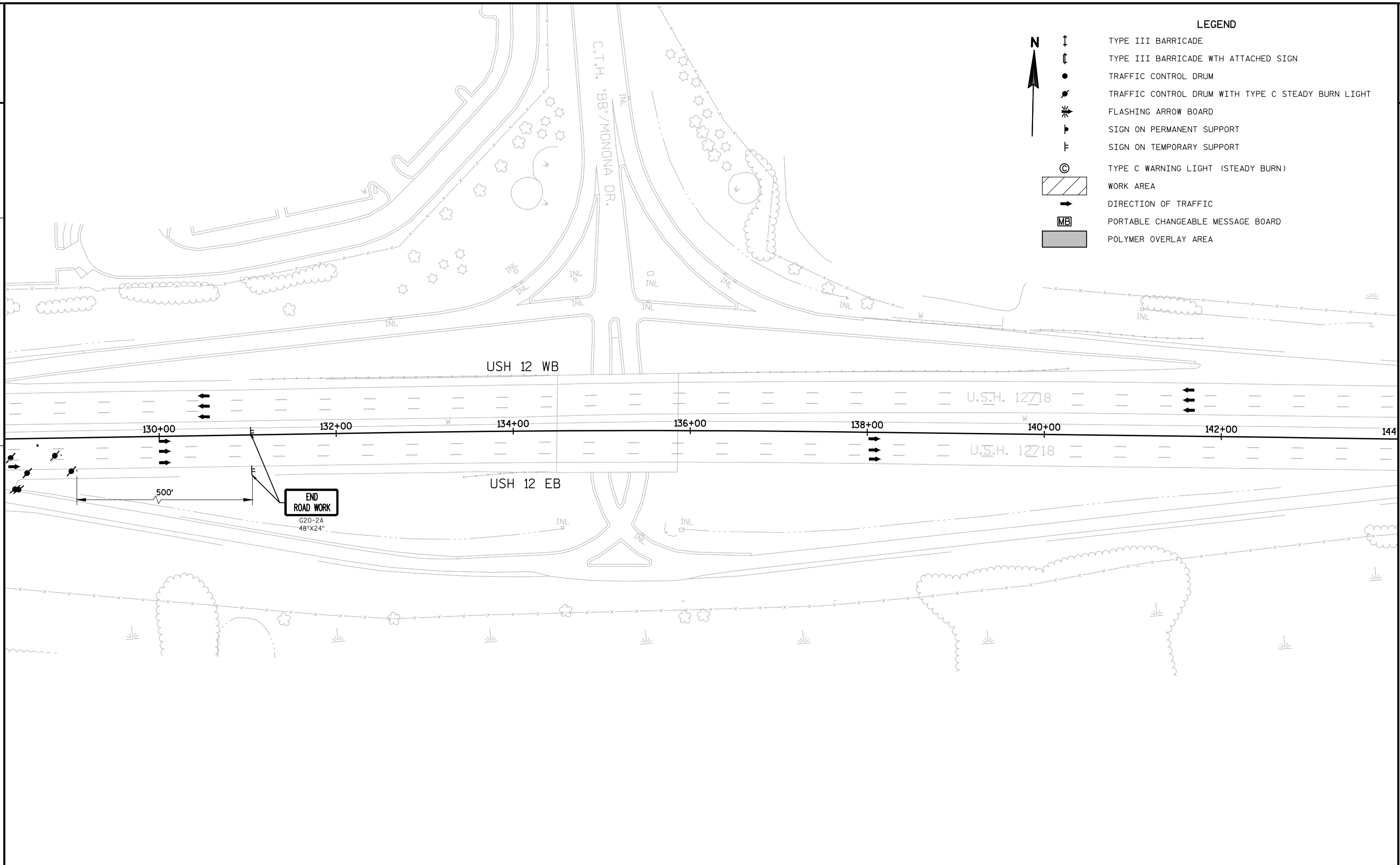


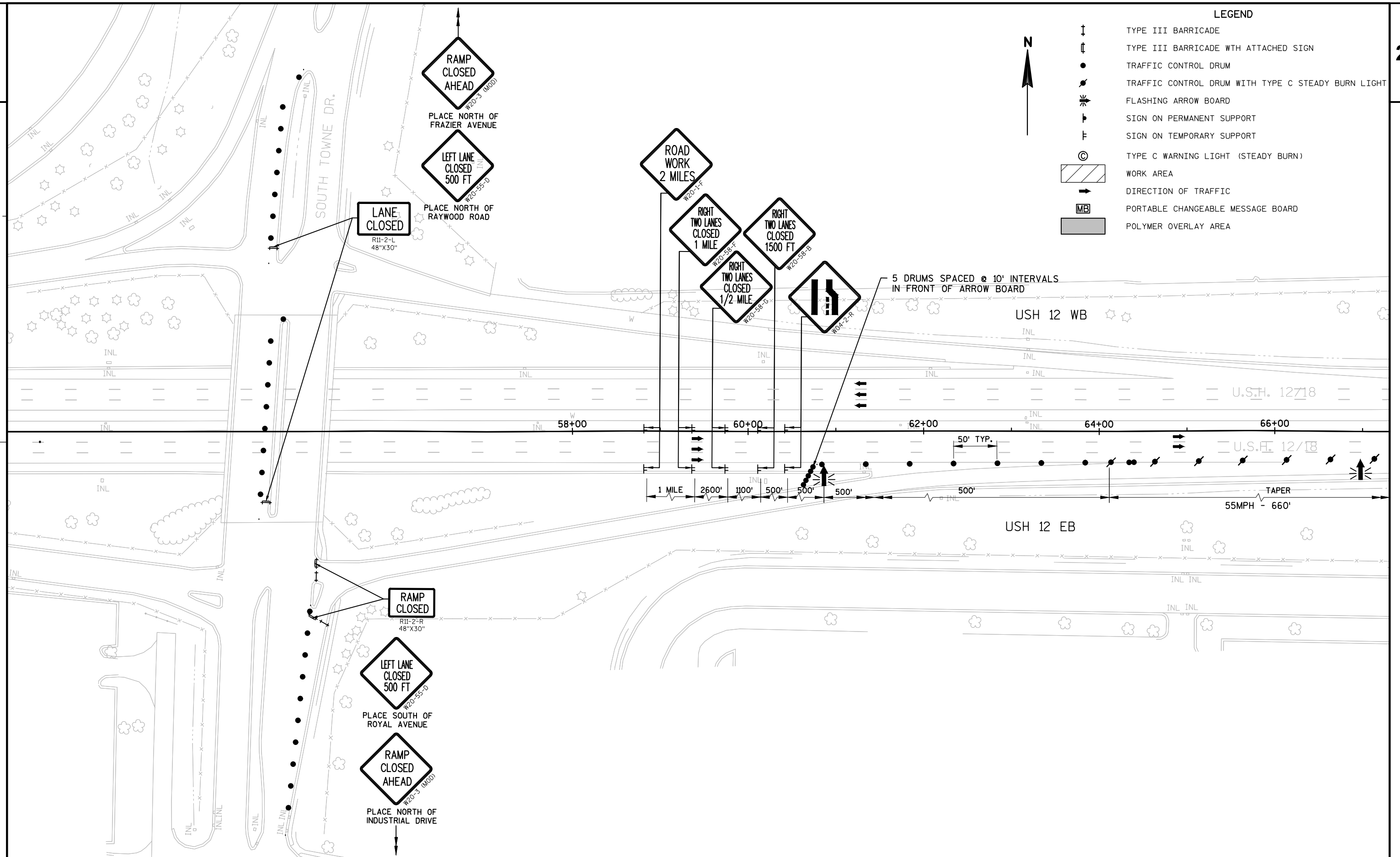


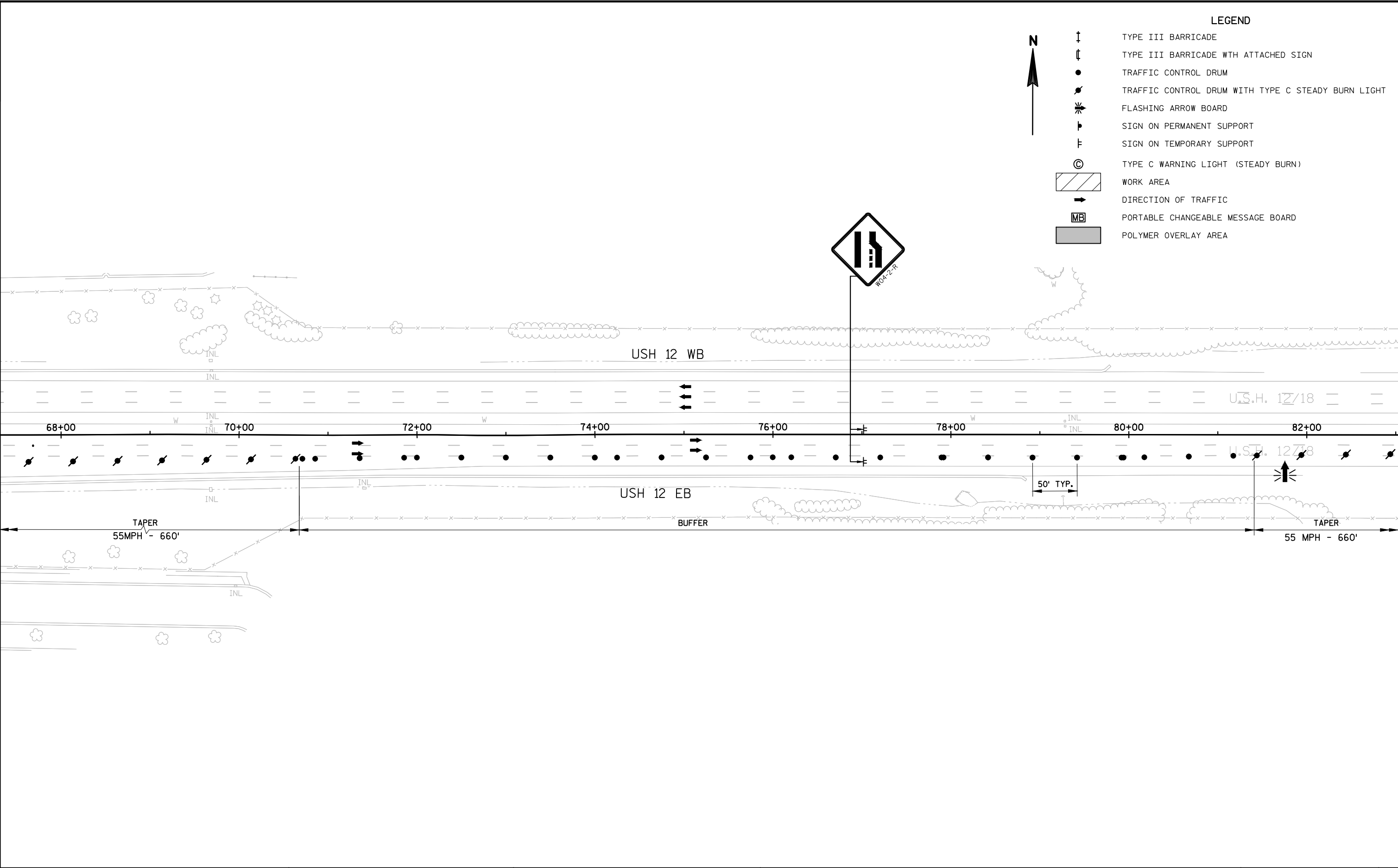
LEGEND

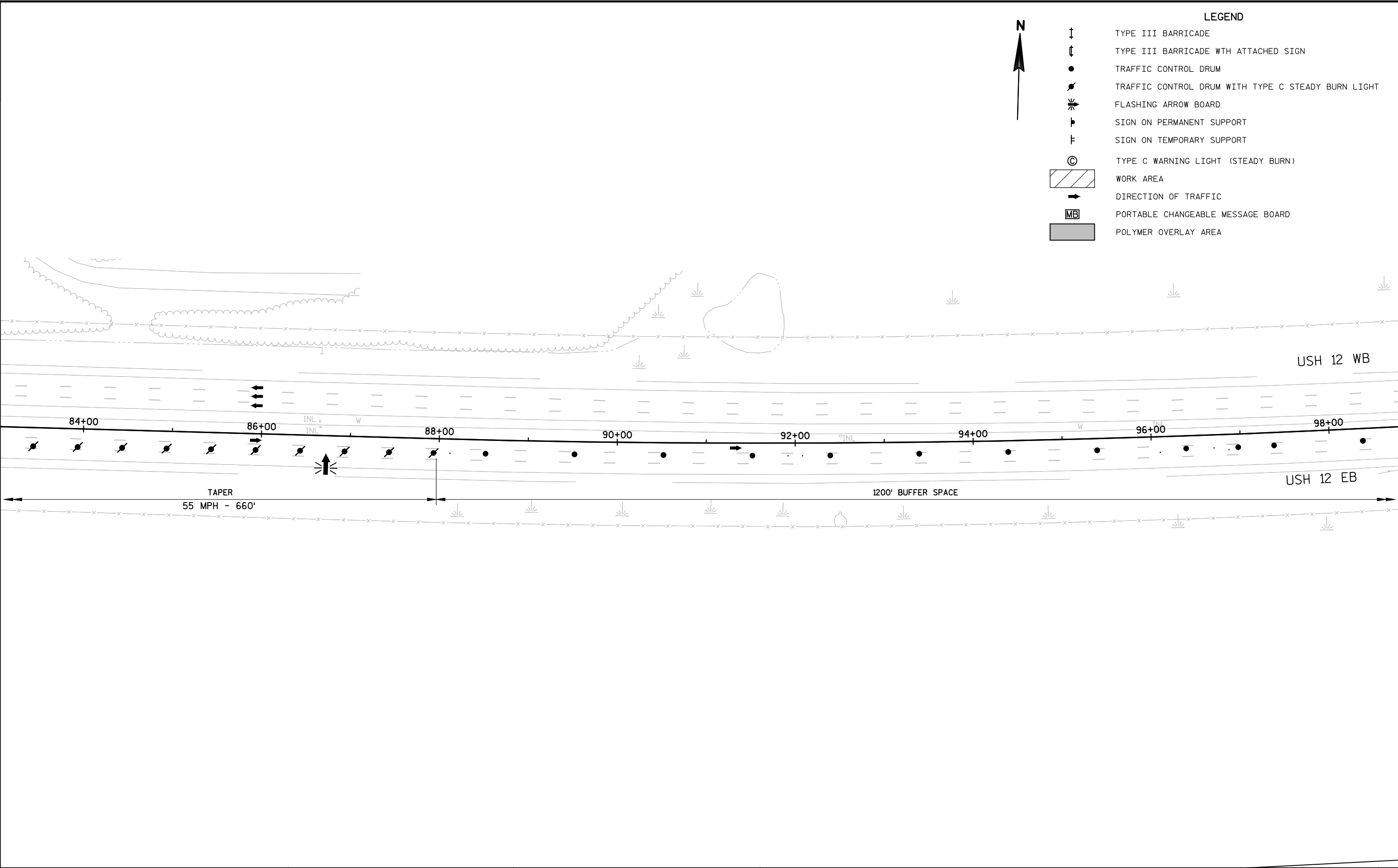
- ↑ TYPE III BARRICADE
- ↑ TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- ⚡ FLASHING ARROW BOARD
- ▮ SIGN ON PERMANENT SUPPORT
- ▮ SIGN ON TEMPORARY SUPPORT
- © TYPE C WARNING LIGHT (STEADY BURN)
- ▨ WORK AREA
- DIRECTION OF TRAFFIC
- MB PORTABLE CHANGEABLE MESSAGE BOARD
- POLYMER OVERLAY AREA

























LEGEND

	TYPE III BARRICADE
	TYPE III BARRICADE WTH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	FLASHING ARROW BOARD
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	TYPE C WARNING LIGHT (STEADY BURN)
	WORK AREA
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD
	POLYMER OVERLAY AREA



USH 12 WB

USH 12 EB

U.S.H. 12/18

116+00

118+00

120+00

122+00

124+00

126+00

128+00

130+00

LANE
CLOSEDR11-2-L
48"X30"

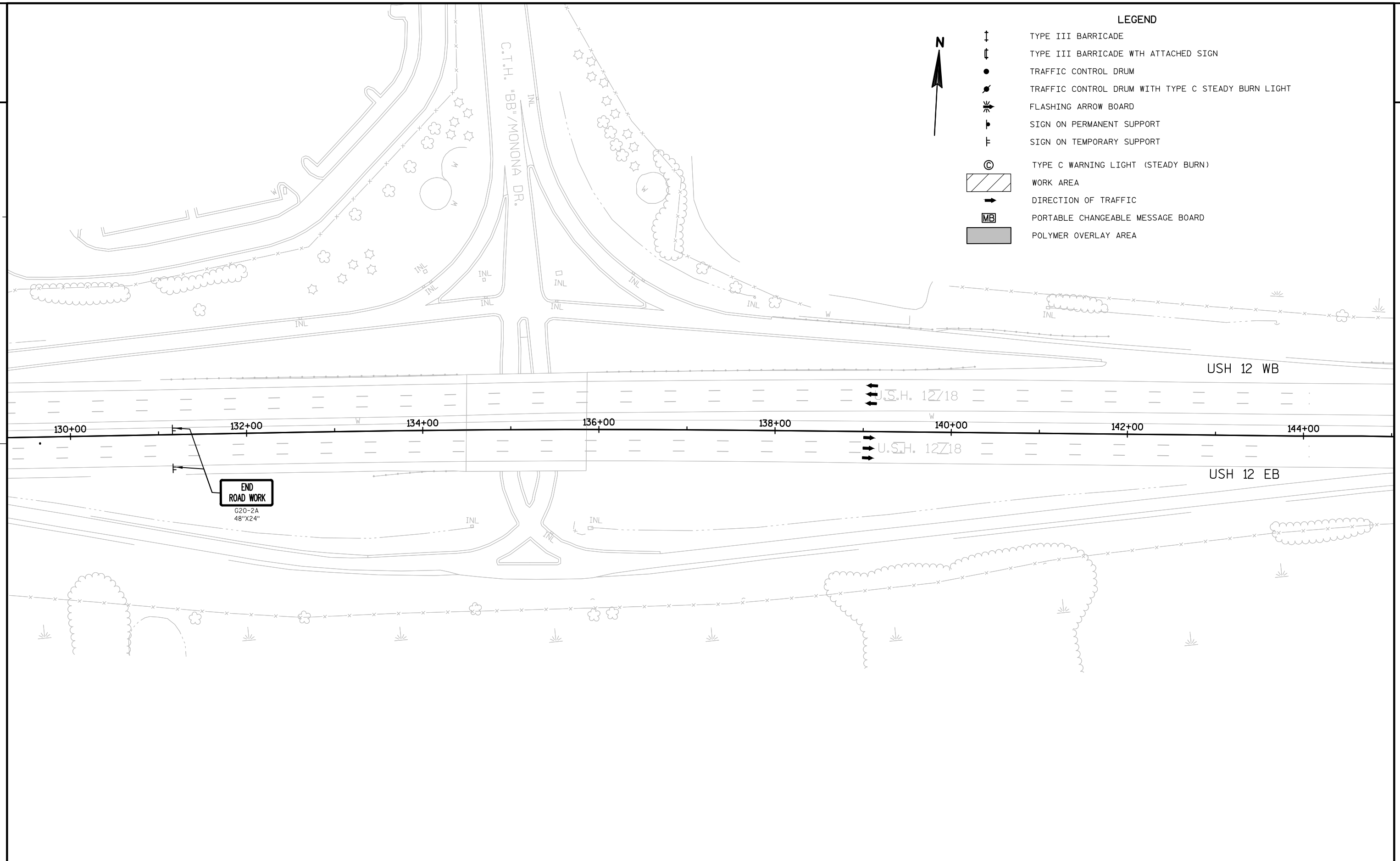
(PLACE 2 BARRICADES AND A "LANE CLOSED" SIGN APPROX.
EVERY ¼ MILE ACROSS THE CLOSED LANES)

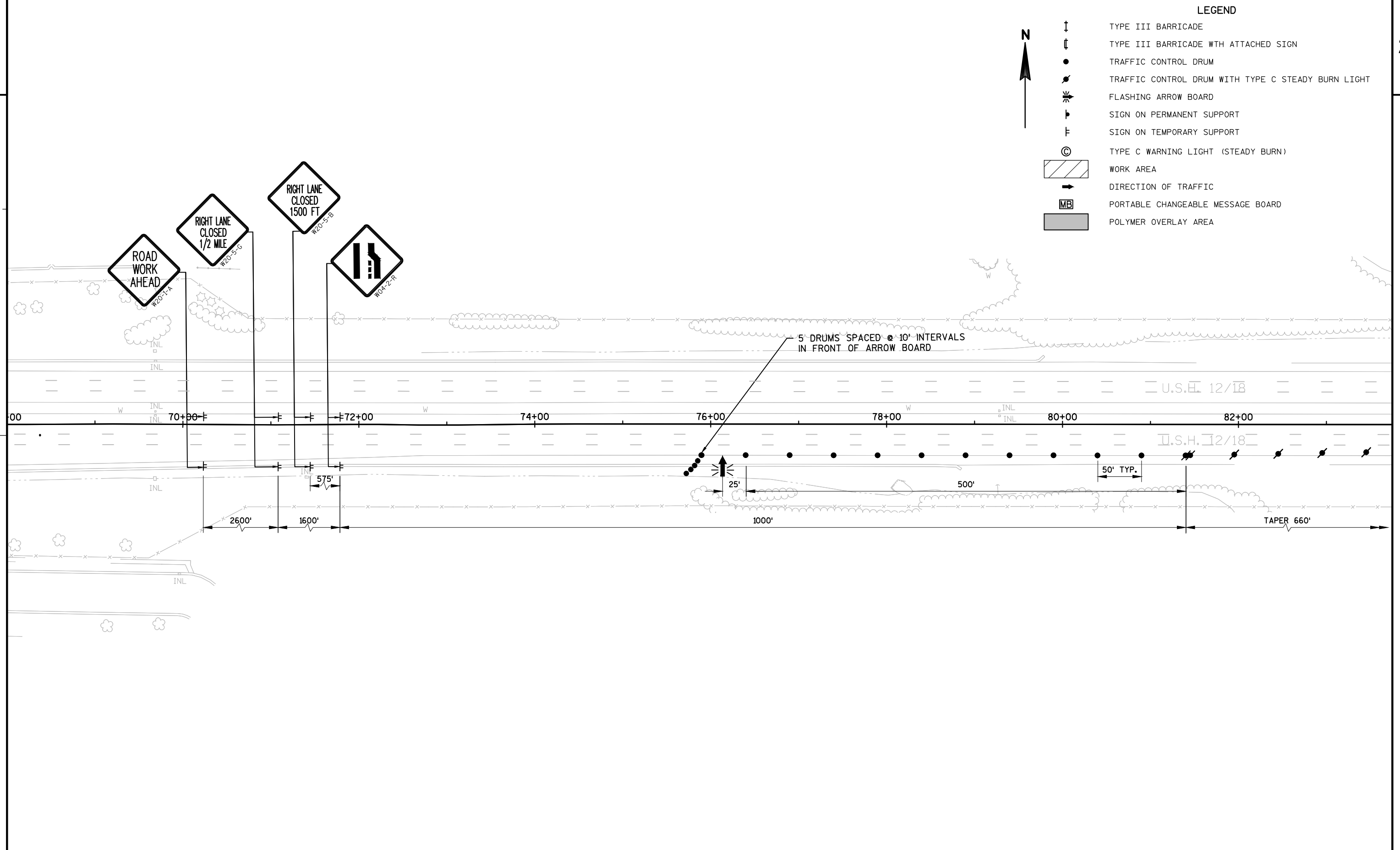
YAHARA RIVER

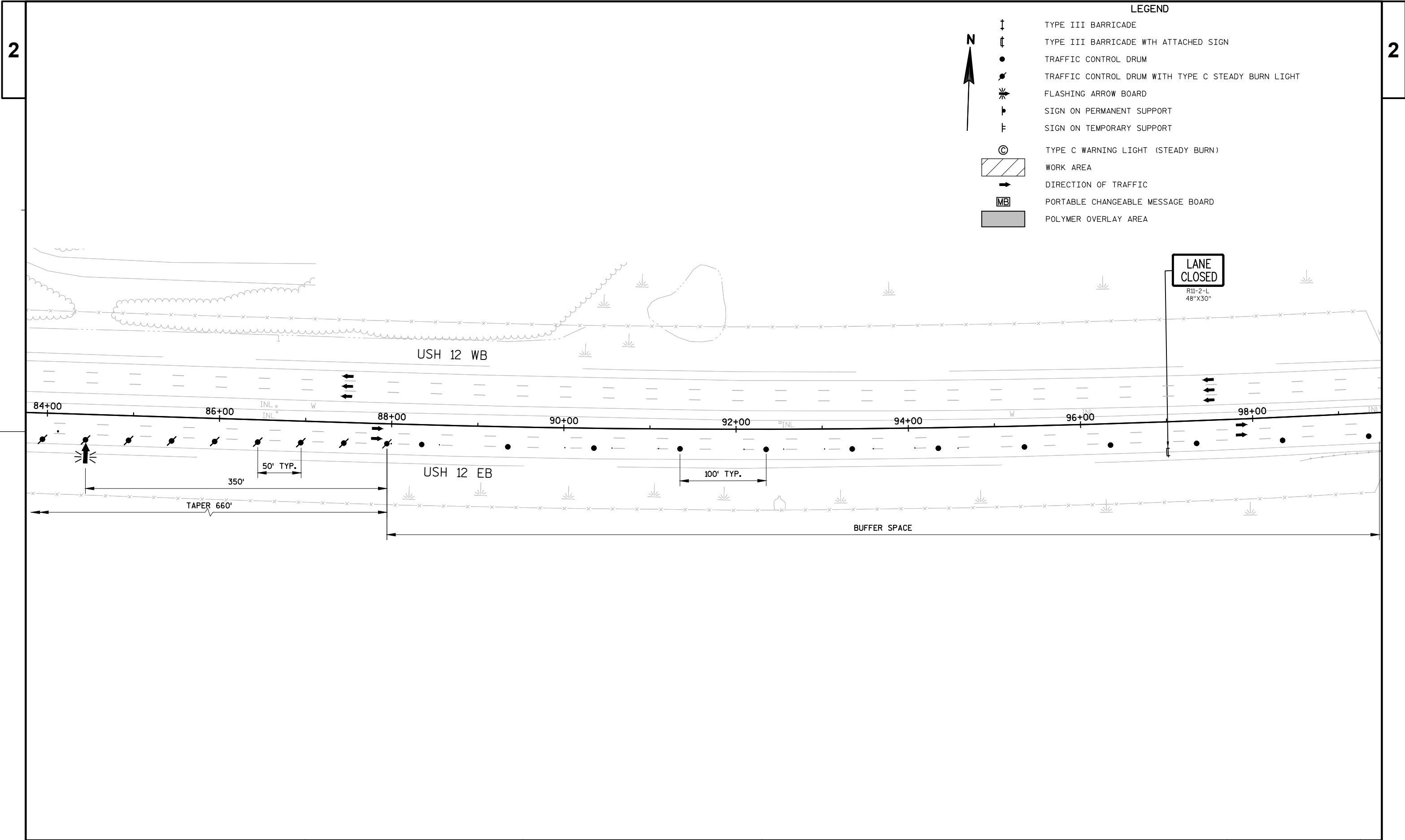
100' TYP.

OVERLAY LANE 3
B-13-315RAMP
CLOSEDR11-2-R
48"X30"EXIT
265

SEE SPECIAL PROVISIONS FOR
ADVANCED NOTICE REQUIREMENTS







LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TYPE C WARNING LIGHT (STEADY BURN)
- WORK AREA
- DIRECTION OF TRAFFIC
- PORTABLE CHANGEABLE MESSAGE BOARD
- POLYMER OVERLAY AREA

PROJECT NO:1206-04-65

HWY:USH 12













COUNTY:DANE

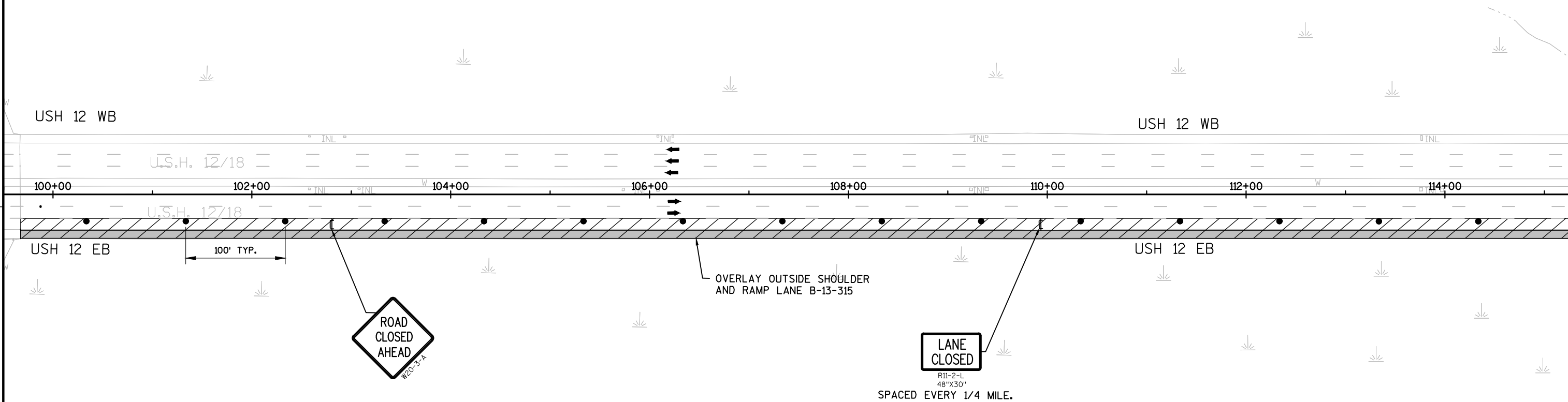
TRAFFIC CONTROL STAGE 5

SHEET













E

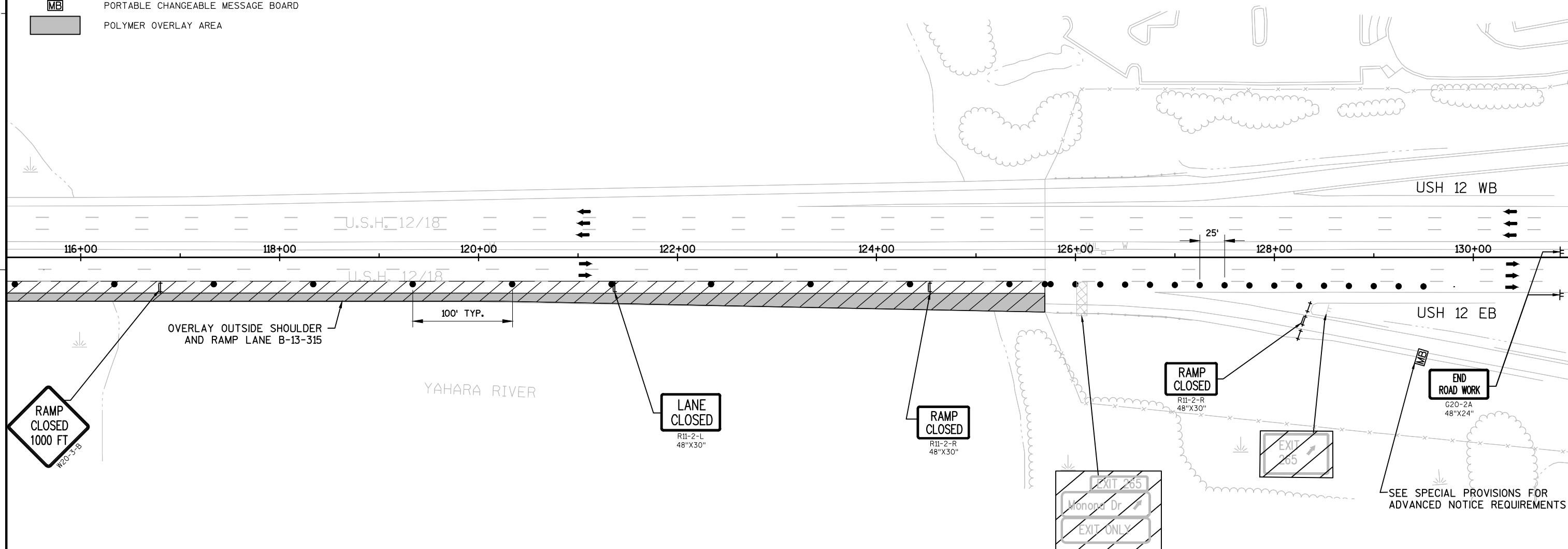
LEGEND

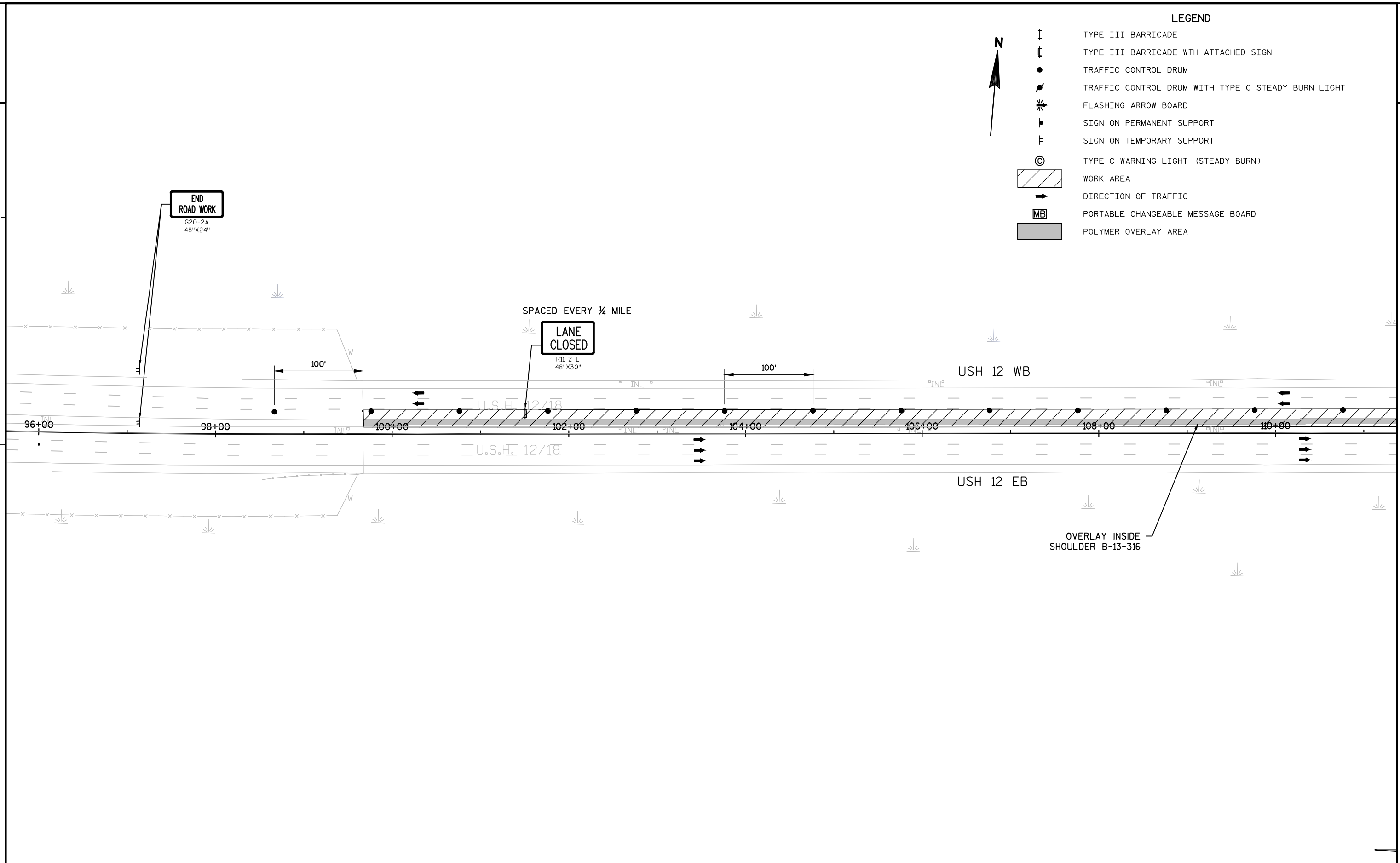
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  FLASHING ARROW BOARD
-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  TYPE C WARNING LIGHT (STEADY BURN)
-  WORK AREA
-  DIRECTION OF TRAFFIC
-  PORTABLE CHANGEABLE MESSAGE BOARD
-  POLYMER OVERLAY AREA















LEGEND

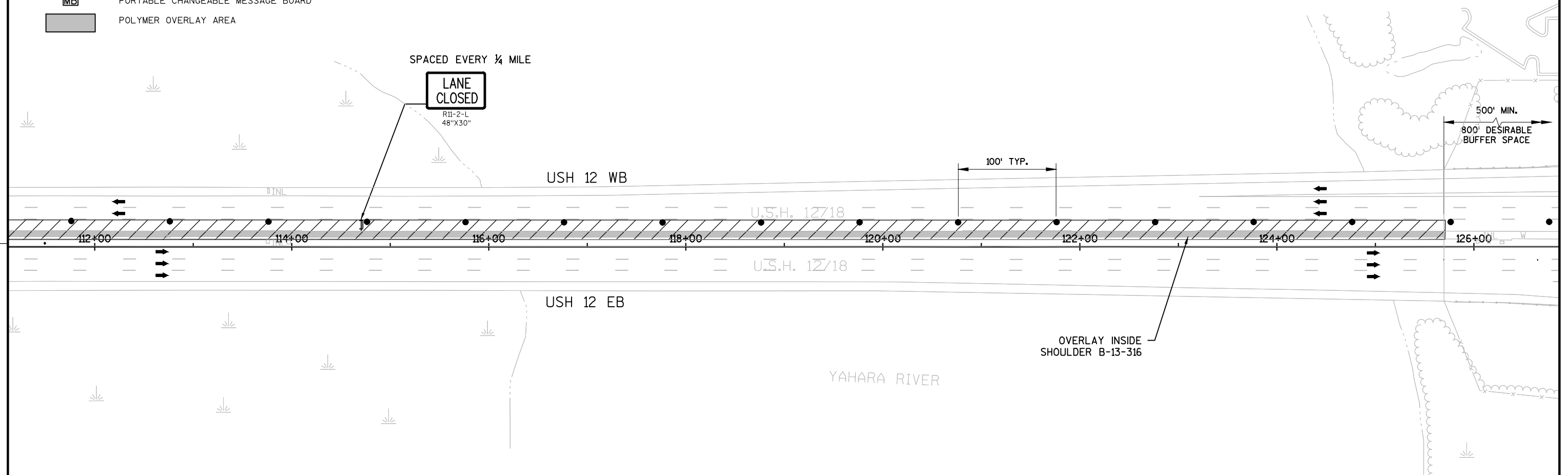
	TYPE III BARRICADE
	TYPE III BARRICADE WTH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	FLASHING ARROW BOARD
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	TYPE C WARNING LIGHT (STEADY BURN)
	WORK AREA
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD
	POLYMER OVERLAY AREA



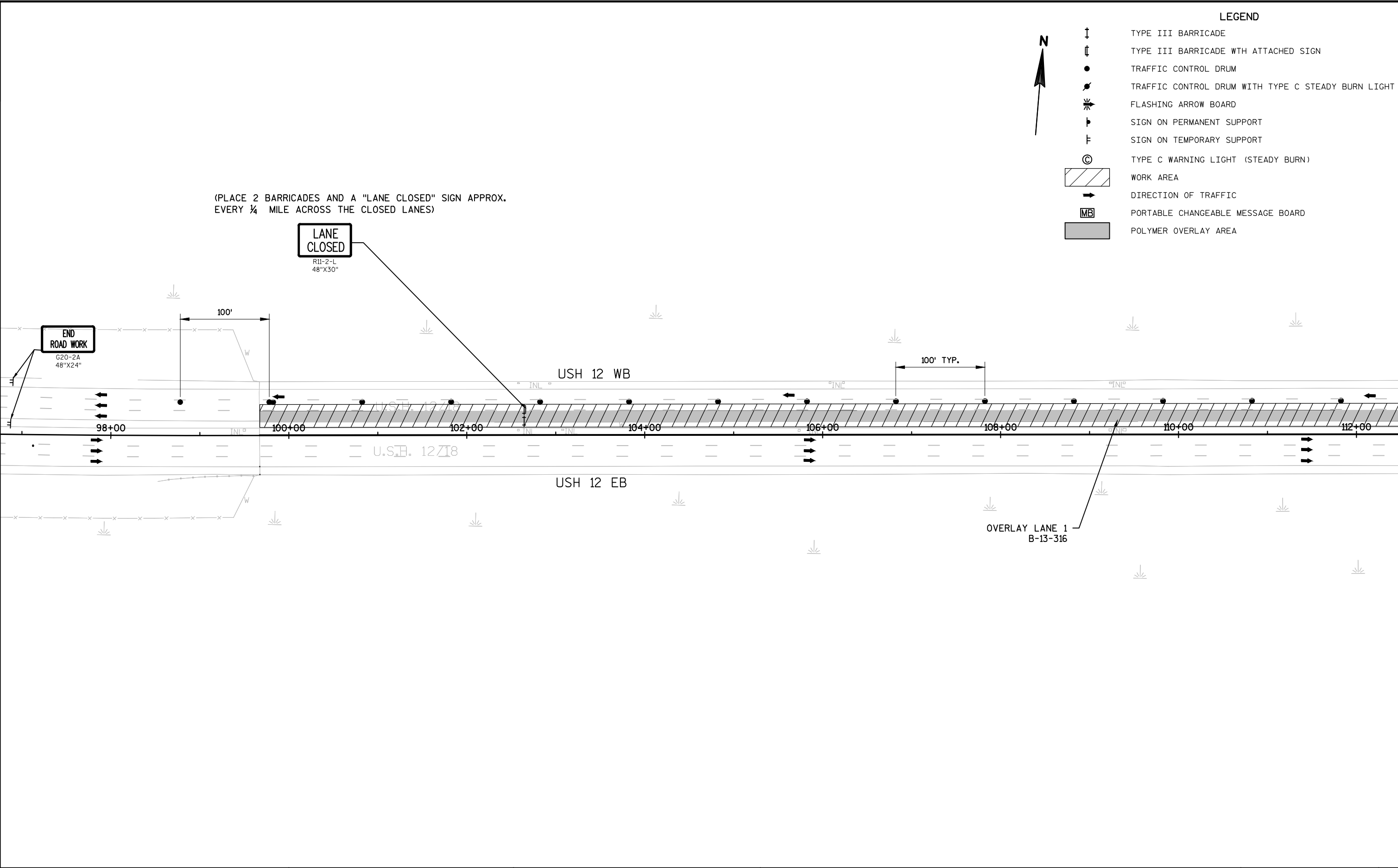


LEGEND

	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	FLASHING ARROW BOARD
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	TYPE C WARNING LIGHT (STEADY BURN)
	WORK AREA
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD
	POLYMER OVERLAY AREA







LEGEND

-
- TYPE III BARRICADE

TYPE III BARRICADE WTH ATTACHED SIGN

TRAFFIC CONTROL DRUM

TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT

FLASHING ARROW BOARD

SIGN ON PERMANENT SUPPORT

SIGN ON TEMPORARY SUPPORT

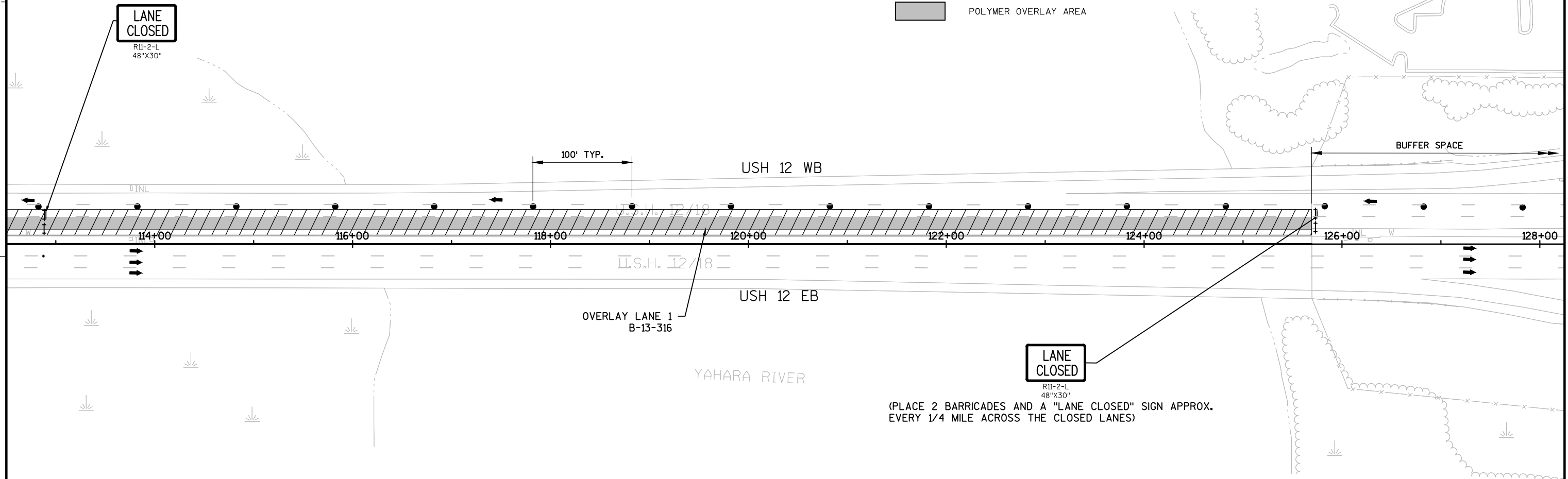
TYPE C WARNING LIGHT (STEADY BURN)

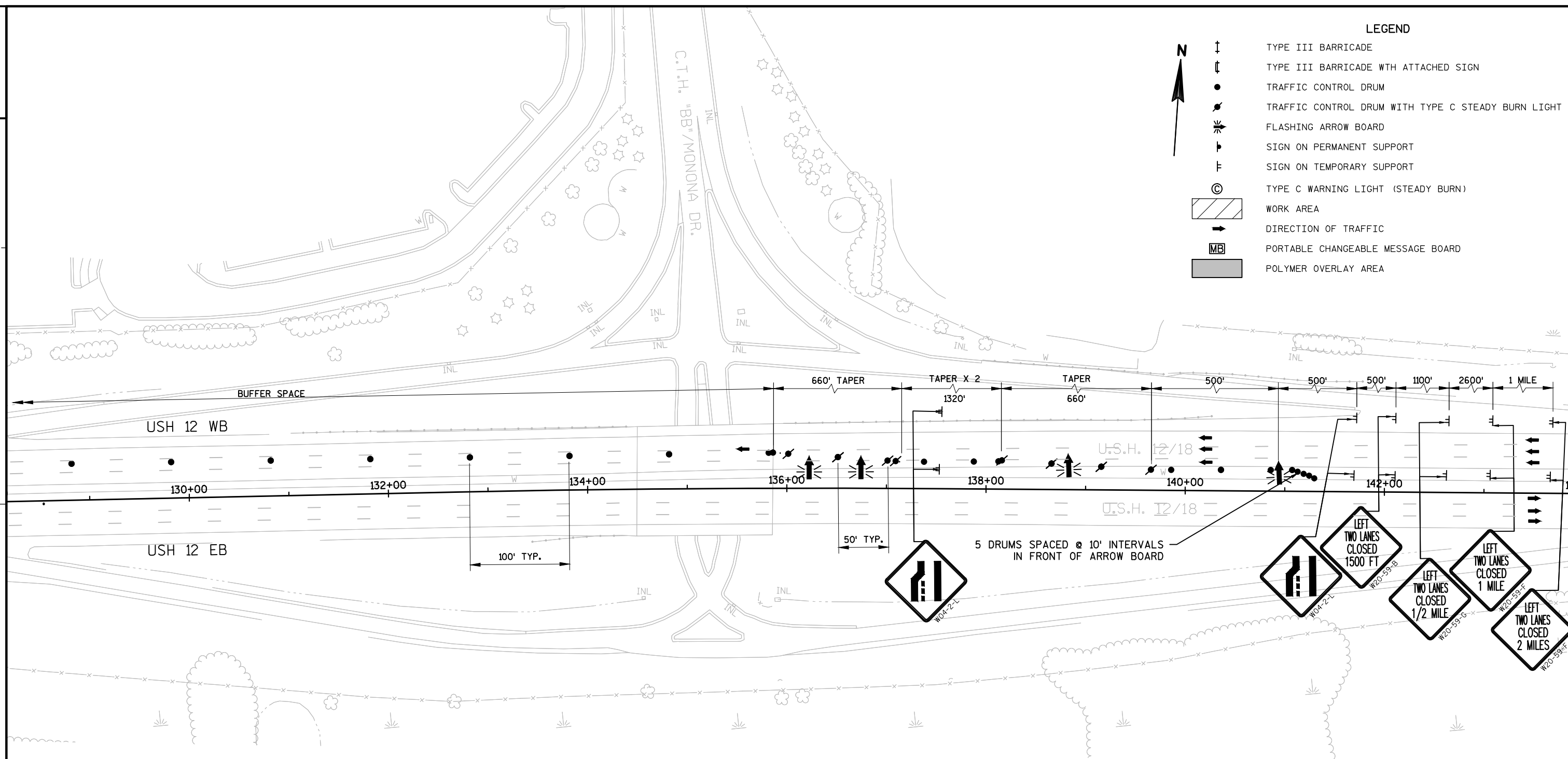
WORK AREA

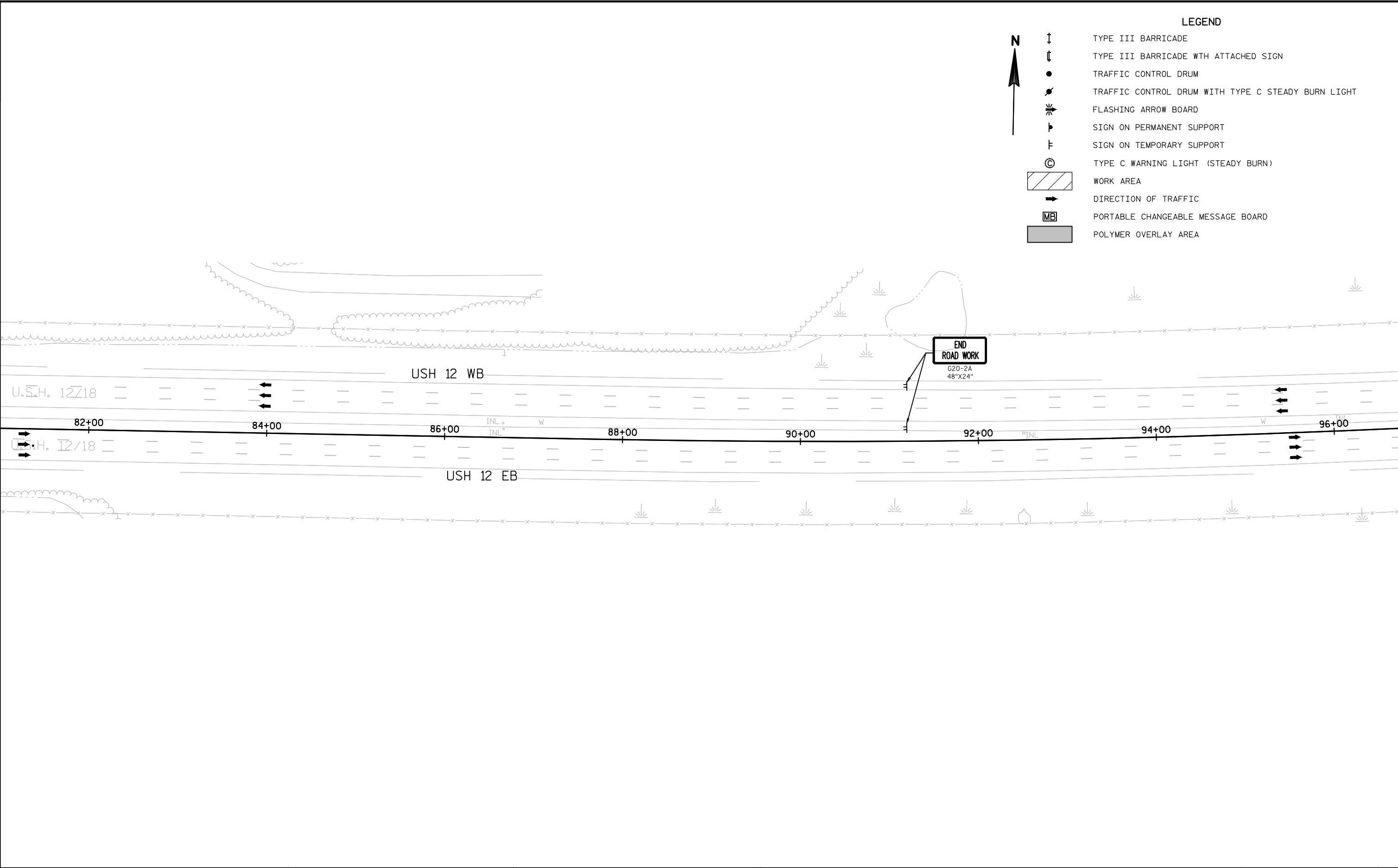
DIRECTION OF TRAFFIC

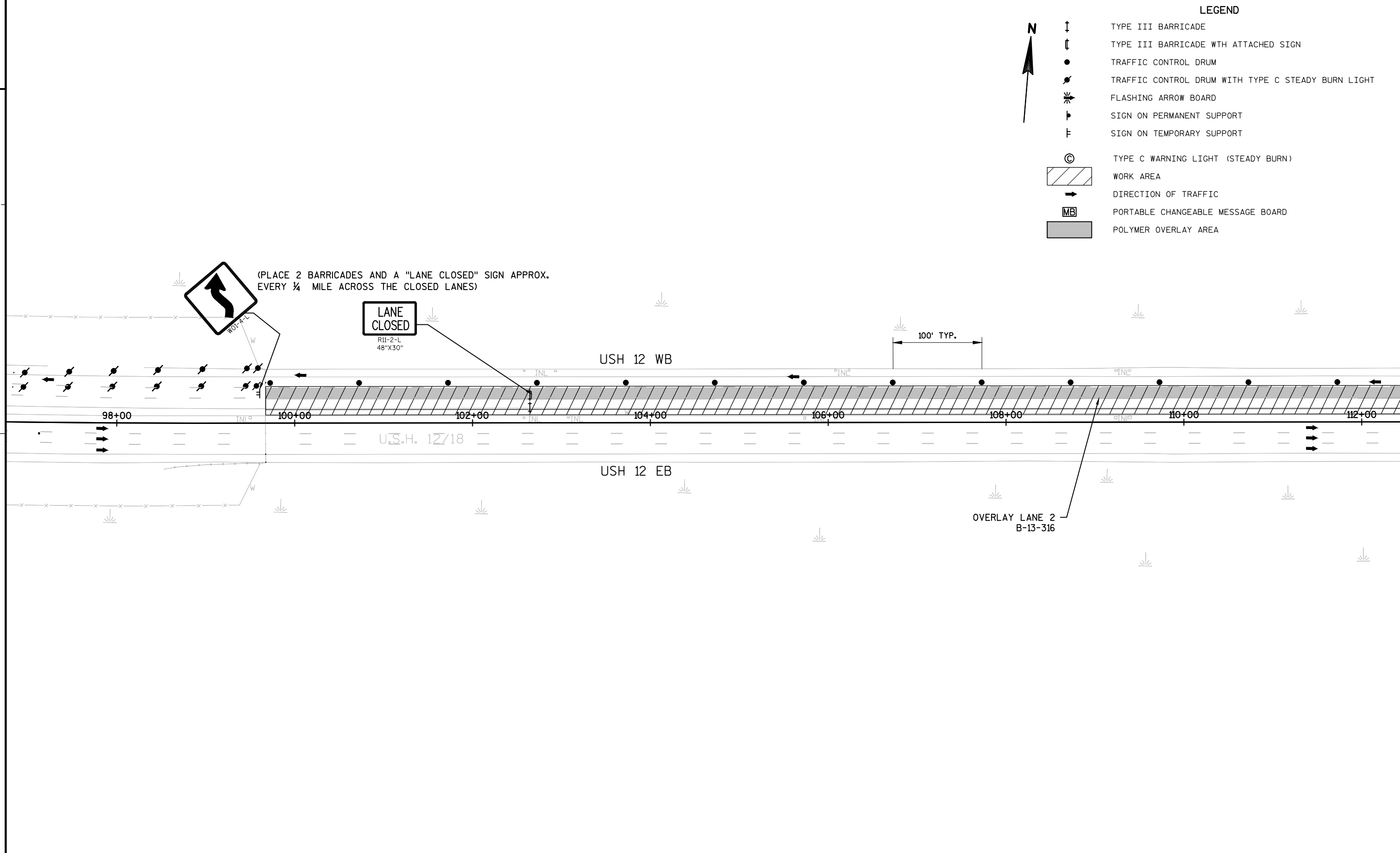
PORTABLE CHANGEABLE MESSAGE BOARD

POLYMER OVERLAY AREA















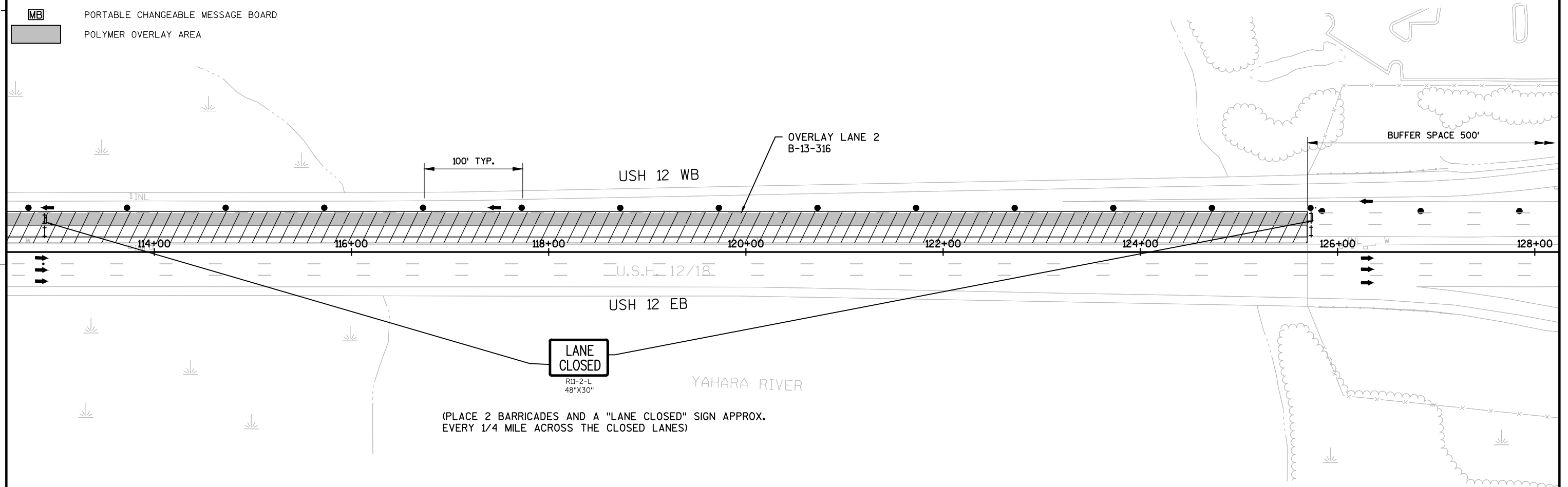


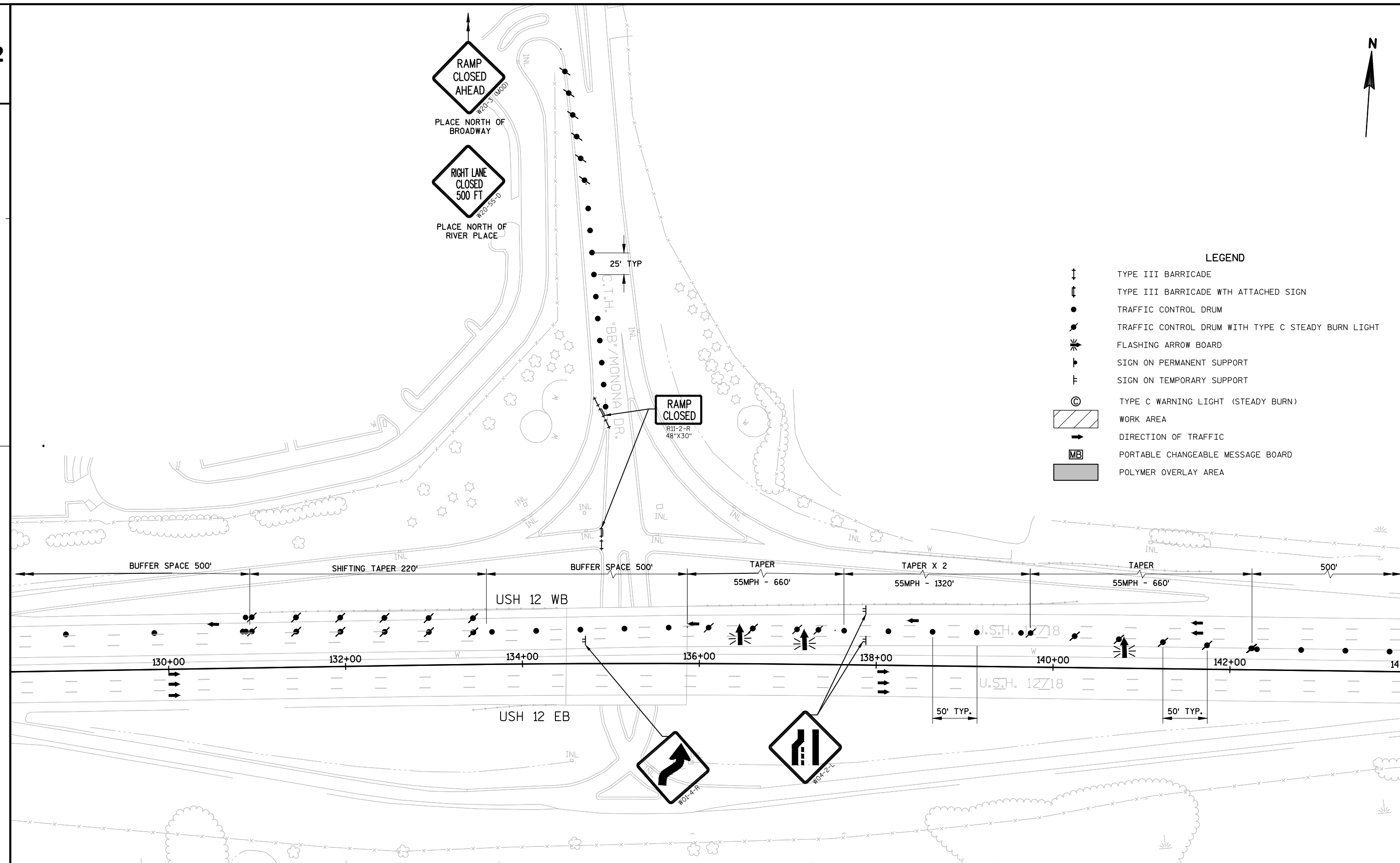




LEGEND

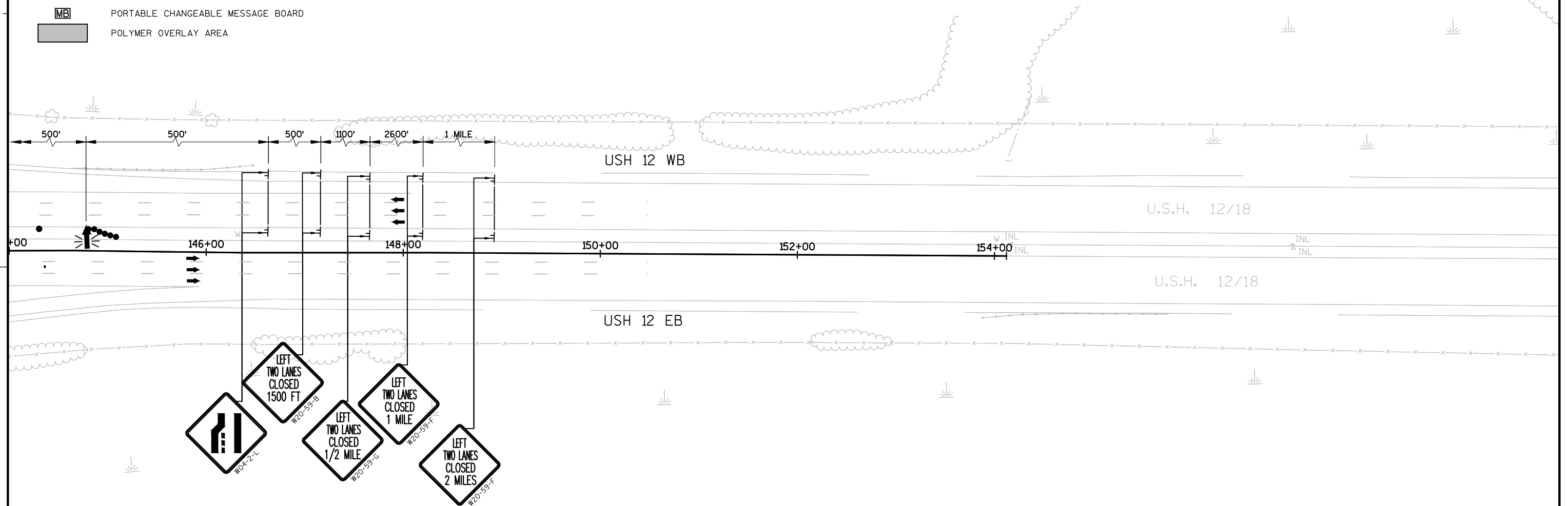
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	FLASHING ARROW BOARD
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	TYPE C WARNING LIGHT (STEADY BURN)
	WORK AREA
	DIRECTION OF TRAFFIC
	PORTABLE CHANGEABLE MESSAGE BOARD
	POLYMER OVERLAY AREA

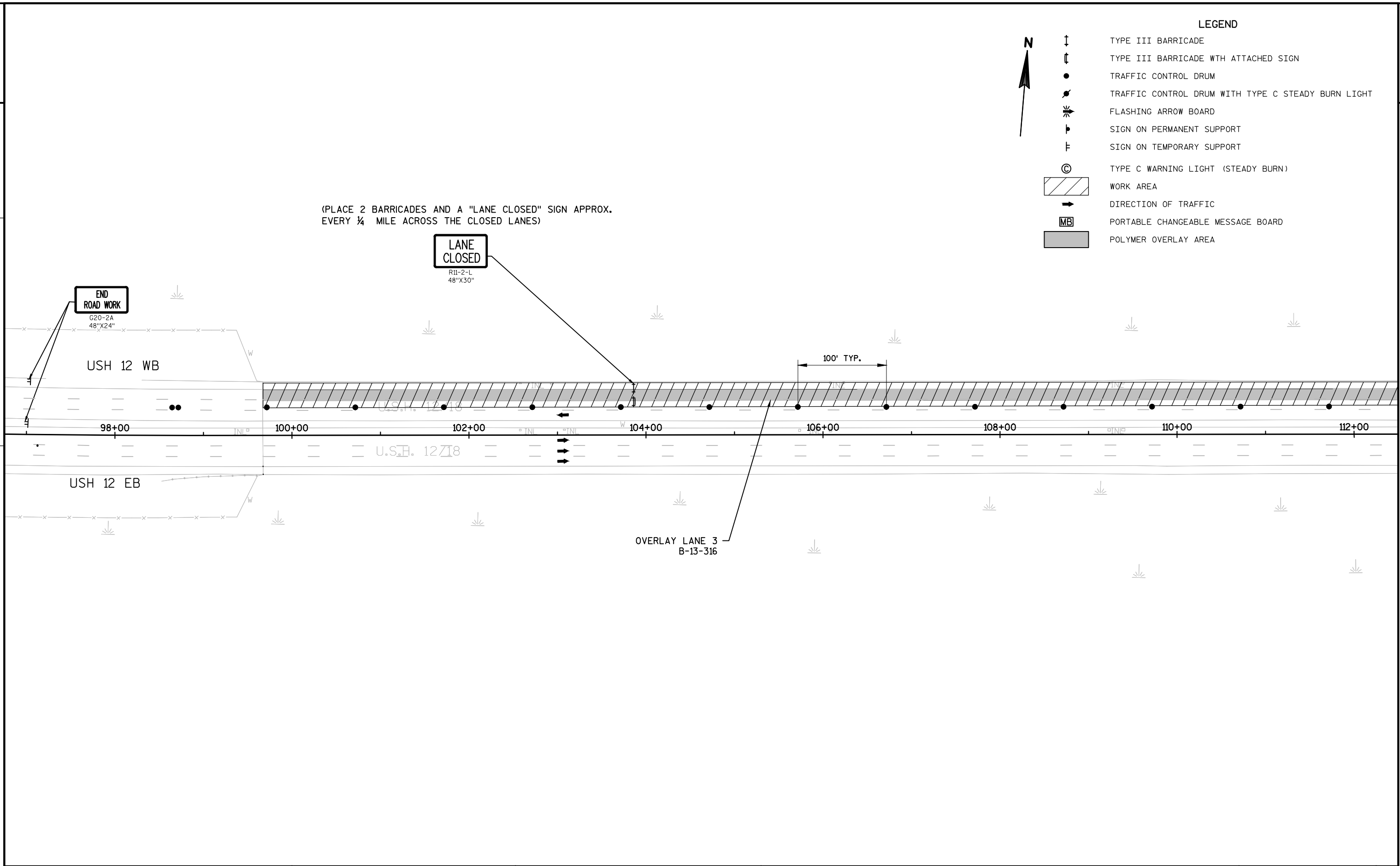


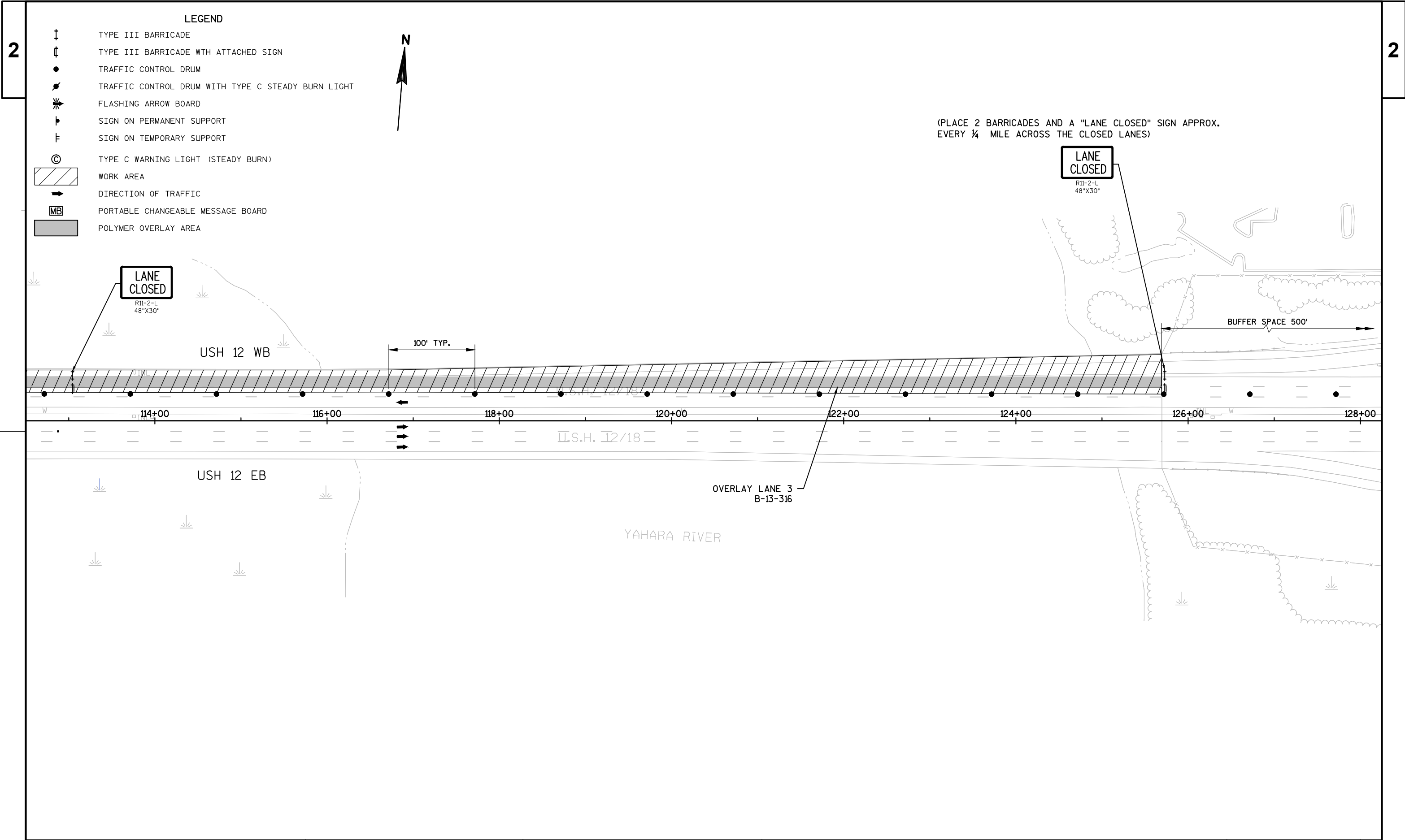


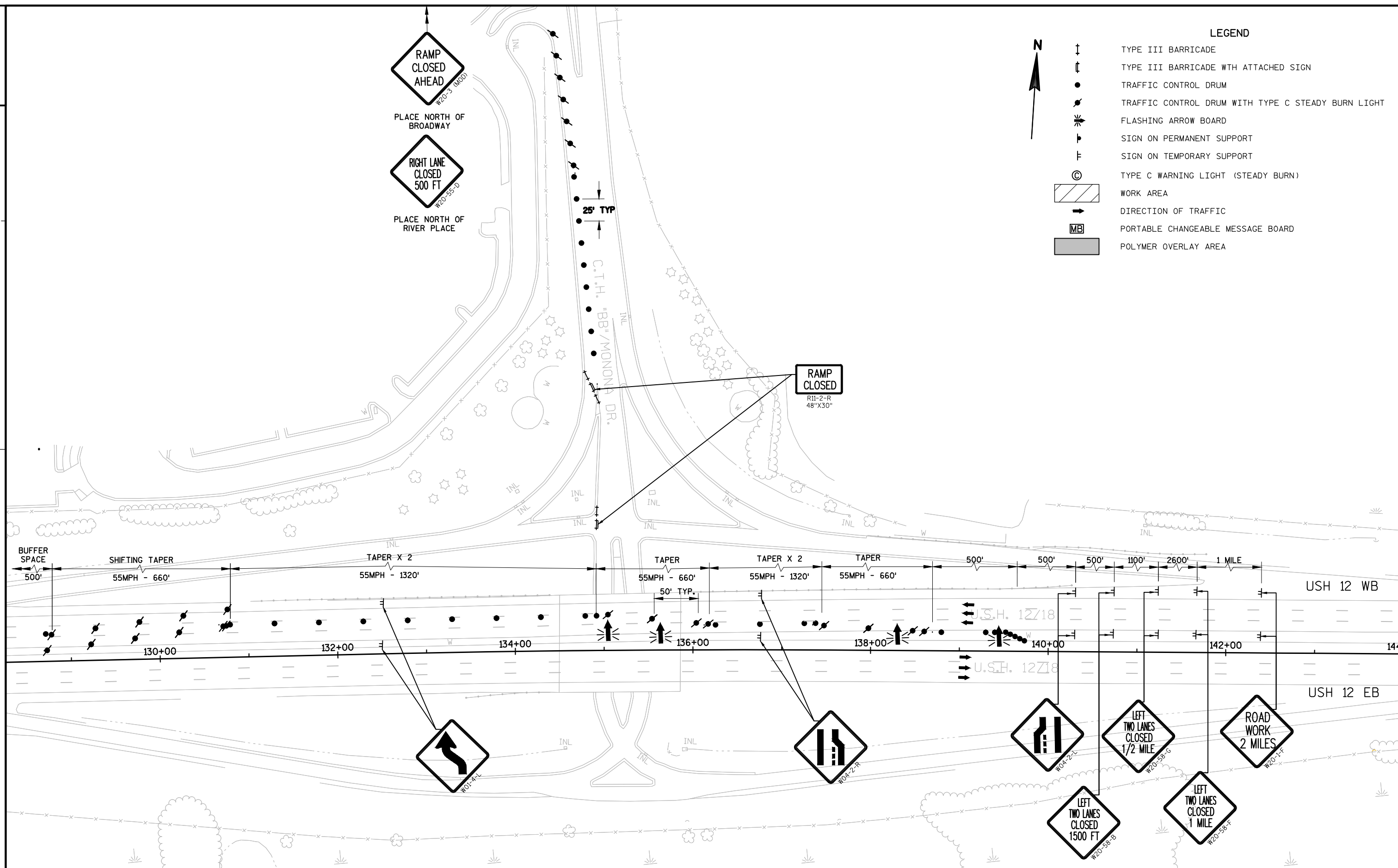
LEGEND

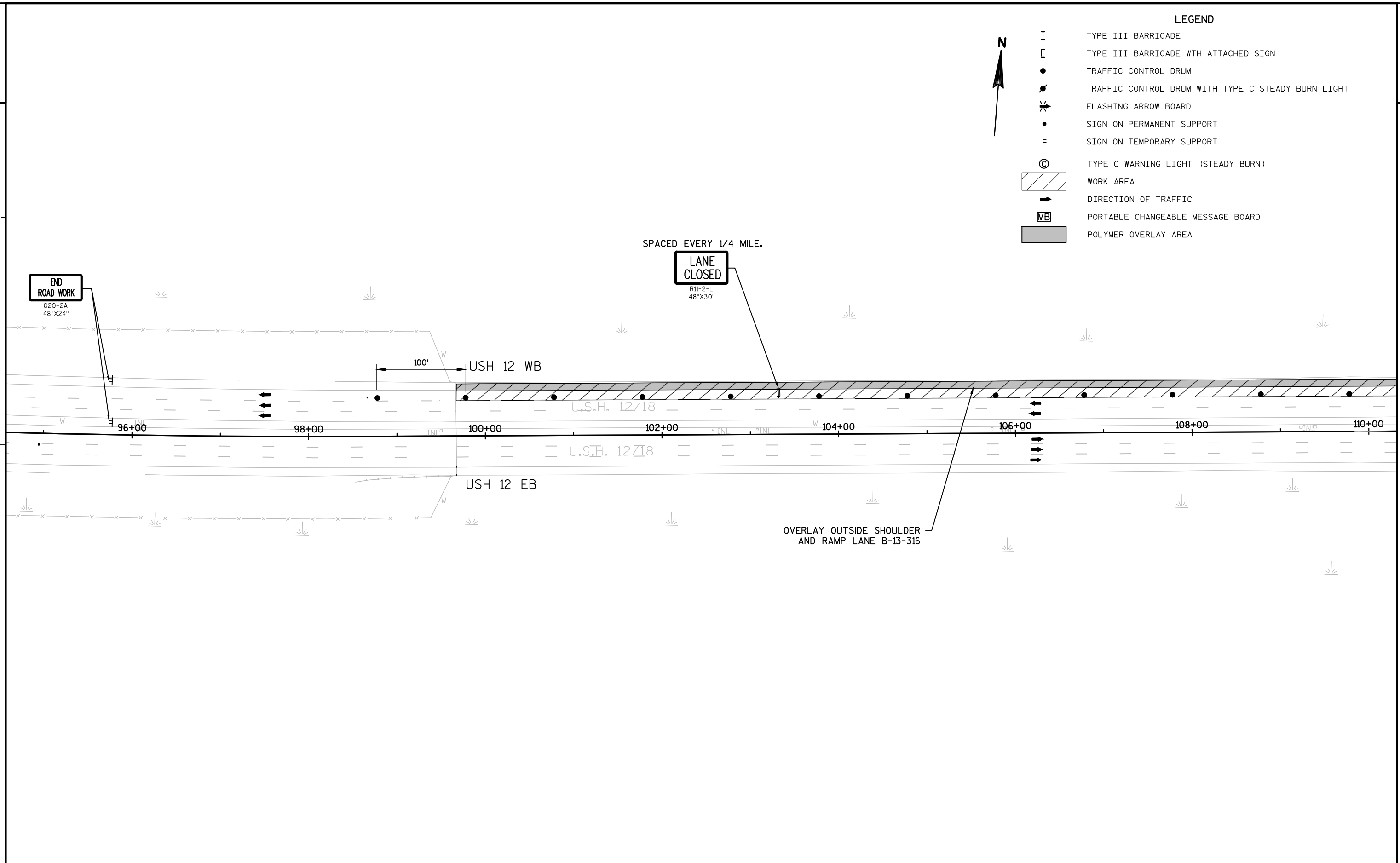
- ↑ TYPE III BARRICADE
- ↑ TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- ⚡ FLASHING ARROW BOARD
- ▬ SIGN ON PERMANENT SUPPORT
- ▬ SIGN ON TEMPORARY SUPPORT
- ⊙ TYPE C WARNING LIGHT (STEADY BURN)
- ▨ WORK AREA
- ➔ DIRECTION OF TRAFFIC
- MB PORTABLE CHANGEABLE MESSAGE BOARD
- POLYMER OVERLAY AREA



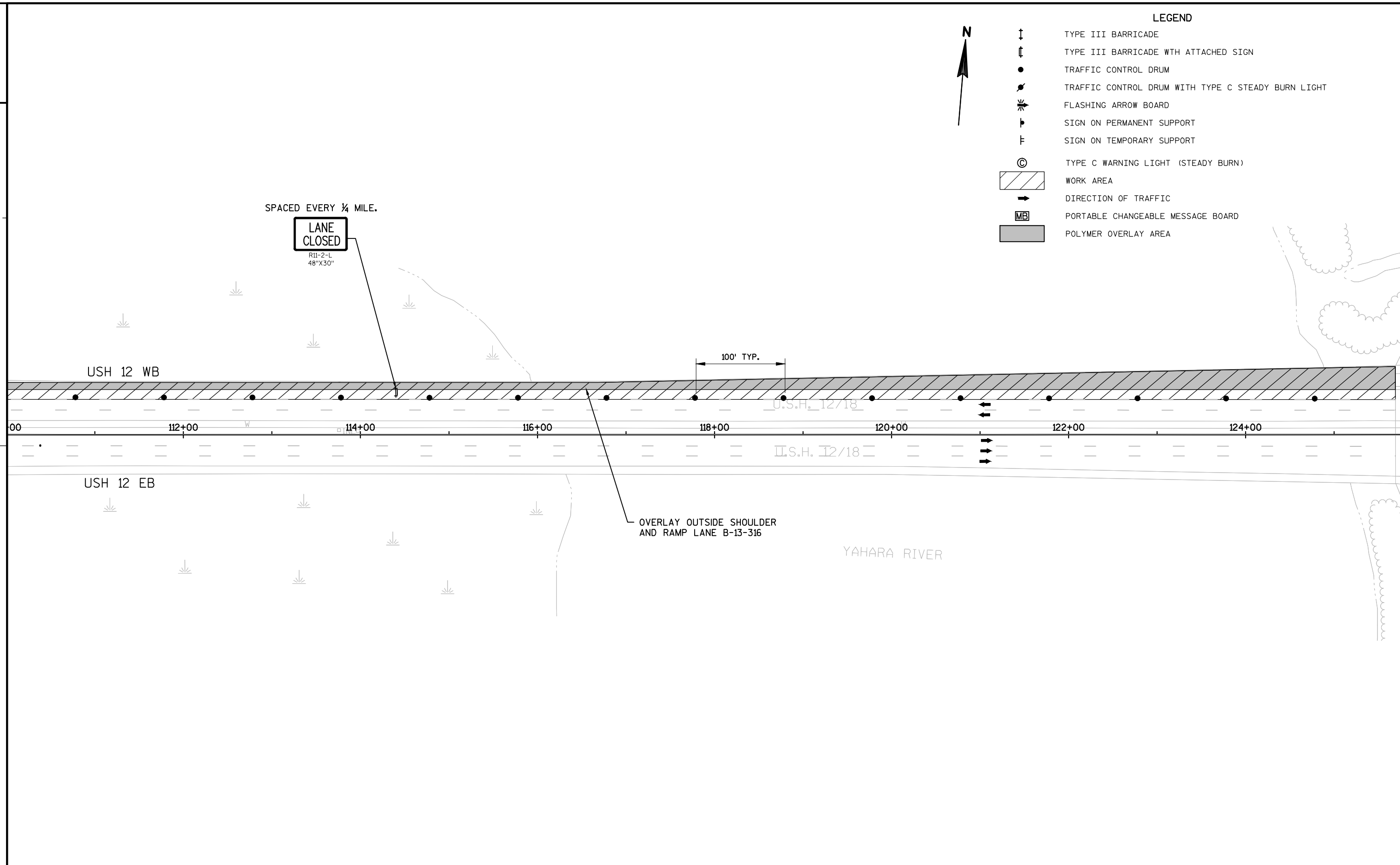




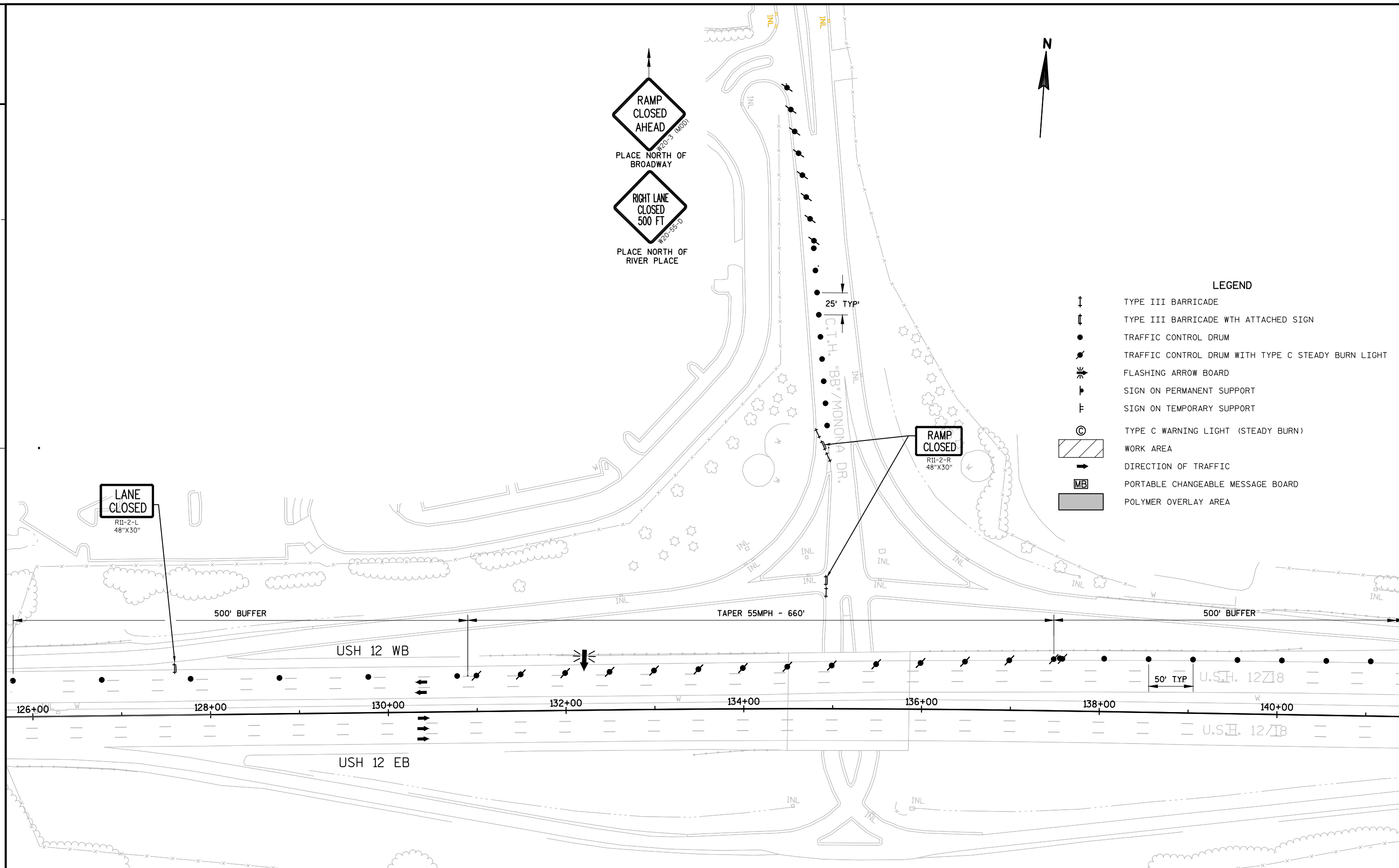


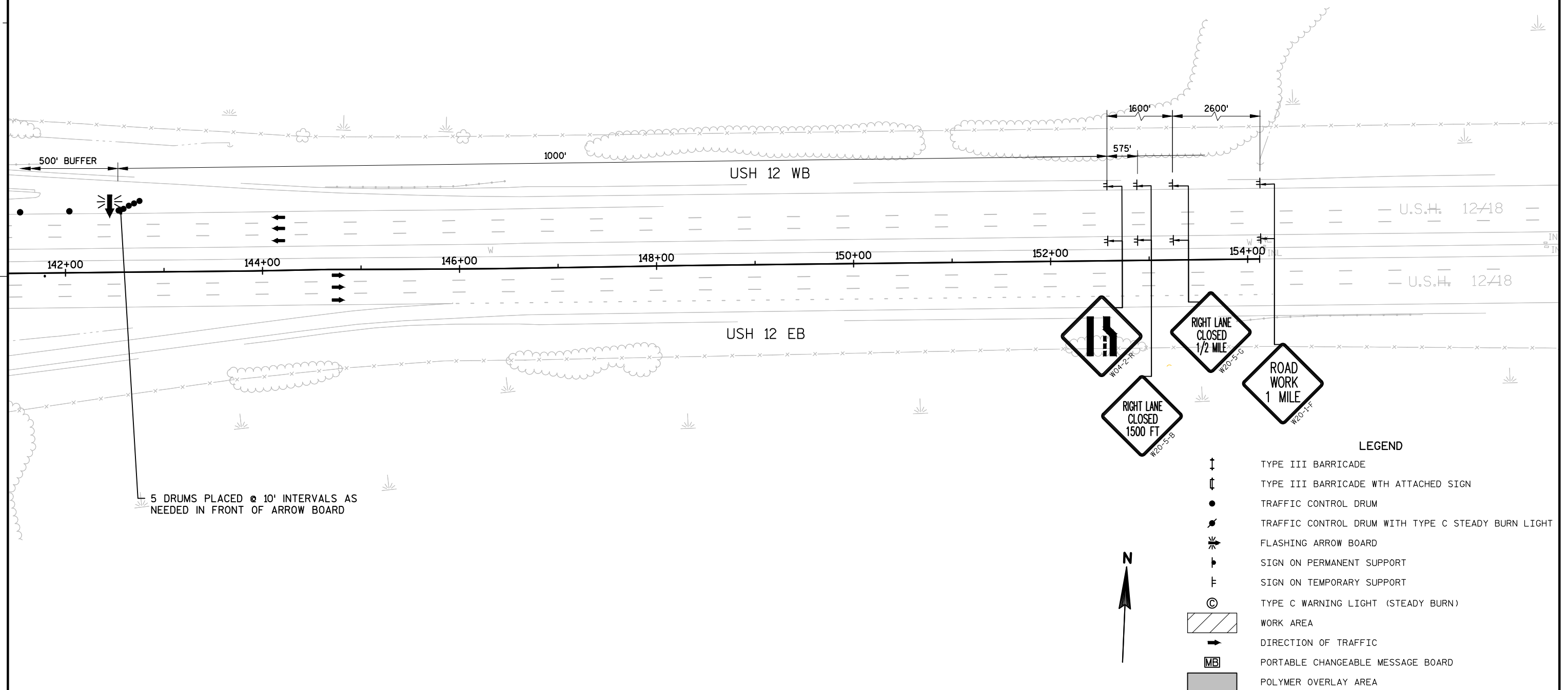


PROJECT NO:1206-40-65	HWY:USH 12	COUNTY:DANE	TRAFFIC CONTROL STAGE 10	SHEET	E
-----------------------	------------	-------------	--------------------------	-------	---



PROJECT NO:1206-40-65	HWY:USH 12	COUNTY:DANE	TRAFFIC CONTROL STAGE 10	SHEET	E
-----------------------	------------	-------------	--------------------------	-------	---





DATE 26FEB16		E S T I M A T E O F Q U A N T I T I E S			
LINE					1206-04-65
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	108.4300	RBC Progress Schedule	EACH	1.000	1.000
0020	213.0100	Finishing Roadway (project) 01. 1206-04-65	EACH	1.000	1.000
0030	509.0301	Preparation Decks Type 1	SY	18.000	18.000
0040	509.0302	Preparation Decks Type 2	SY	8.000	8.000
0050	618.0100	Maintenance And Repair of Haul Roads (project) 01. 1206-04-65	EACH	1.000	1.000
0060	619.1000	Mobilization	EACH	1.000	1.000
0070	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0080	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0090	628.7010	Inlet Protection Type B	EACH	21.000	21.000
0100	643.0200	Traffic Control Surveillance and Maintenance (project) 01. 1206-04-65	DAY	54.000	54.000
0110	643.0300	Traffic Control Drums	DAY	6,550.000	6,550.000
0120	643.0420	Traffic Control Barricades Type III	DAY	452.000	452.000
0130	643.0715	Traffic Control Warning Lights Type C	DAY	2,155.000	2,155.000
0140	643.0800	Traffic Control Arrow Boards	DAY	188.000	188.000
0150	643.0900	Traffic Control Signs	DAY	1,094.000	1,094.000
0160	643.0920	Traffic Control Covering Signs Type II	EACH	11.000	11.000
0170	643.1050	Traffic Control Signs PCMS	DAY	139.000	139.000
0180	646.0106	Pavement Marking Epoxy 4-Inch	LF	13,009.000	13,009.000
0190	646.0126	Pavement Marking Epoxy 8-Inch	LF	500.000	500.000
0200	SPV.0025	Special 01. Polyester Polymer Concrete Masonry	CF	38.000	38.000
0210	SPV.0060	Special 01. Repositioning Traffic Control Devices For Mainline Closures	EACH	34.000	34.000
0220	SPV.0060	Special 02. Traffic Control Close-Open Freeway Entrance Ramp	EACH	18.000	18.000
0230	SPV.0090	Special 01. Sawing Pavement Deck Preparation Areas	LF	180.000	180.000
0240	SPV.0180	Special 01. Rapid Cure Polymer Overlay	SY	30,177.000	30,177.000

TRAFFIC CONTROL																					
643.0200 TRAFFIC CONTROL SURVEILLANCE AND MAINTENANCE 1206-04-65 DAY		643.0300 TRAFFIC CONTROL DRUMS EACH DAYS		643.0420 TRAFFIC CONTROL BARRICADES TYPE III EACH DAYS		643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C EACH DAYS		643.0800 TRAFFIC CONTROL ARROW BOARDS EACH DAYS		643.0900 TRAFFIC CONTROL SIGNS EACH DAYS		643.0920 TRAFFIC CONTROL COVERING SIGNS TYPE II EACH		643.1050 TRAFFIC CONTROL SIGNS PCMS EACH DAY		SPV.0060.01 REPOSITIONING TRAFFIC CONTROL DEVICES FOR MAINLINE CLOSURES EACH		SPV.0060.02 TRAFFIC CONTROL CLOSE-OPEN FREEWAY ENTRANCE RAMP EACH		NO. OF CLOSURES REMARKS	
STAGE																					
SOUNDING EB (INSIDE LANES)	1	135	135	6	6	72	72	4	4	20	20	-	1	1	1	-	1	1	DAYS		
SOUNDING EB (OUTSIDE LANES)	1	150	150	18	18	45	45	4	4	30	30	1	2	2	1	-	1	1	DAYS		
SOUNDING WB (INSIDE LANES)	1	150	150	6	6	52	52	4	4	23	23	-	1	1	1	1	1	1	DAYS		
SOUNDING WB (OUTSIDE LANES)	1	172	172	12	12	60	60	4	4	23	23	-	2	2	1	1	1	1	DAYS		
1	3	68	204	3	9	14	42	2	6	13	39	-	1	5	1	-	1	3	DAYS		
2	6	107	642	6	36	27	162	4	24	17	102	-	1	8	4	-	1	6	DAYS		
3	6	135	810	6	36	72	432	4	24	20	120	-	1	8	4	-	1	6	DAYS		
4	6	150	900	18	108	45	270	4	24	30	180	6	2	16	4	-	1	6	DAYS		
5	4	82	328	8	32	14	56	2	8	18	72	4	2	12	2	-	1	4	DAYS		
6	3	67	201	3	9	14	42	2	6	13	39	-	1	5	1	-	1	3	DAYS		
7	6	101	606	6	36	27	162	4	24	17	102	-	1	8	4	-	1	6	DAYS		
8	6	150	900	6	36	52	312	4	24	23	138	-	1	8	4	6	1	6	DAYS		
9	6	172	1,032	12	72	60	360	4	24	23	138	-	2	16	4	6	1	6	DAYS		
10	4	80	320	9	36	22	88	2	8	17	68	-	2	12	2	4	1	4	DAYS		
ONE WEEK ADVANCED WARNING	-	-	-	-	-	-	-	-	-	-	-	-	5	35	-	-	1	7	DAYS		
TOTALS	54	6,550		452		2,155		188		1,094		11		139		34		18			

EROSION CONTROL

STATION		628.7010 INLET PROTECTION TYPE B EACH
BEGIN PROJECT	100+00.00	1
100+00.00	110+00.00	9
110+00.00	120+00.00	3
120+00.00	130+00.00	3
UNDISTRIBUTED		5
		21

MOBILIZATIONS EROSION CONTROL

628.1905 MOBILIZATIONS EROSION CONTROL EACH		628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
USH 12 EB	2	1
USH 12 WB	2	1
TOTAL	4	2

PAVEMENT MARKING

STATION - STATION		646.0106 EPOXY 4- INCH		646.0126 EPOXY 8- INCH	
		SOLID WHITE LF	SOLID YELLOW LF	DASH WHITE LF	WHITE LF
USH 12 EB					
BEGIN PROJECT	100+00.00	32	32	17	-
100+00.00	110+00.00	1000	1000	500	-
110+00.00	120+00.00	1000	1000	500	-
120+00.00	END PROJECT	569	569	285	
USH 12 WB					
BEGIN PROJECT	100+00.00	33	33	17	-
100+00.00	110+00.00	1000	1000	500	-
110+00.00	120+00.00	1000	1000	500	-
120+00.00	END PROJECT	569	569	285	500
TOTAL		5203	5203	2603	500
13,009					

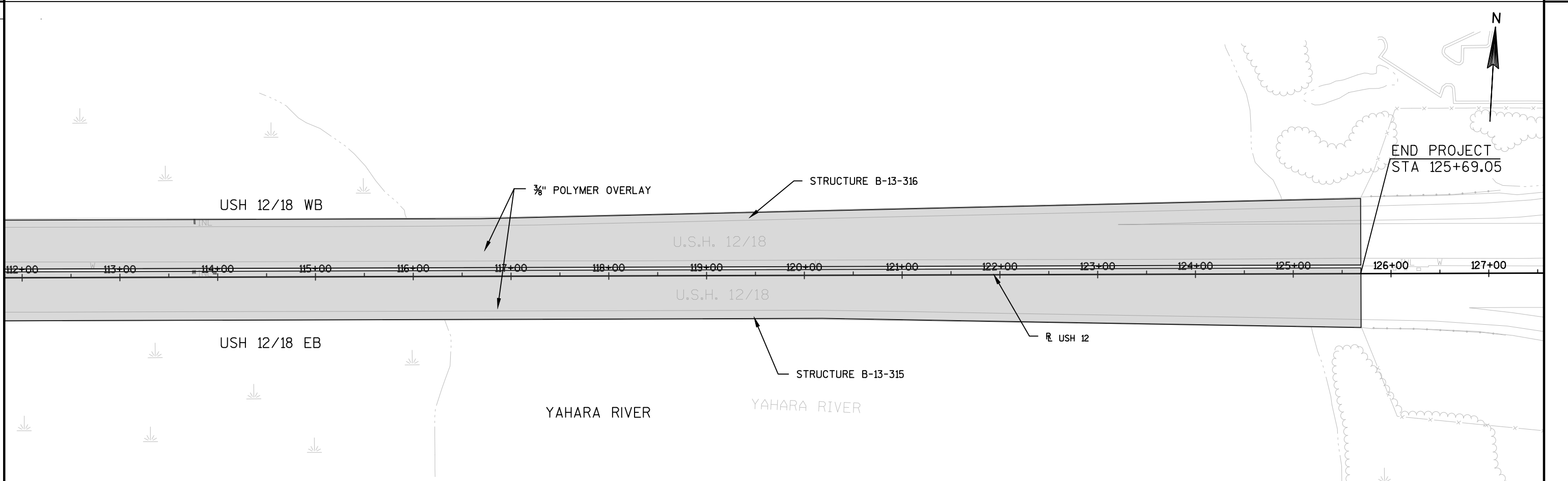
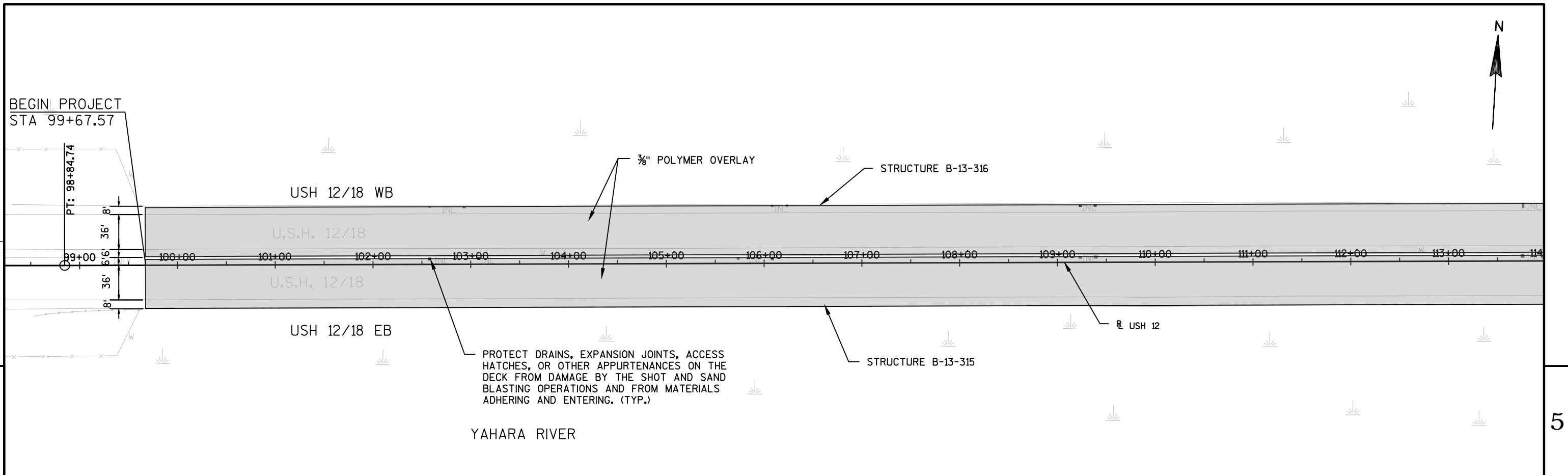
MAINTENANCE AND
REPAIR OF HAUL ROADS

618.0100 1206-04-65	
LOCATION	EACH
PROJECT	1
1	

FINISHING ROADWAY

213.0100 1206-04-65	
LOCATION	EACH
PROJECT	1
1	

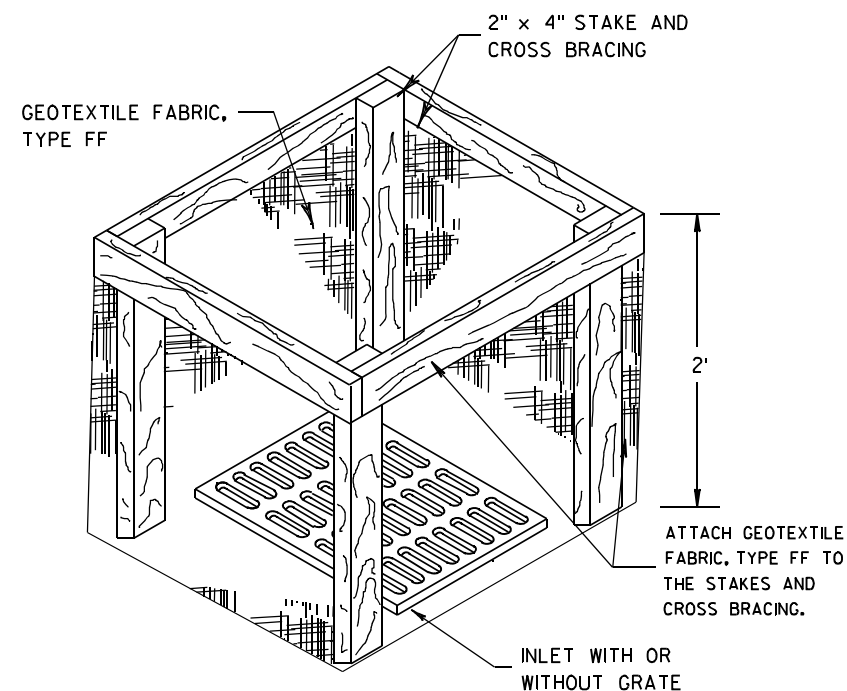
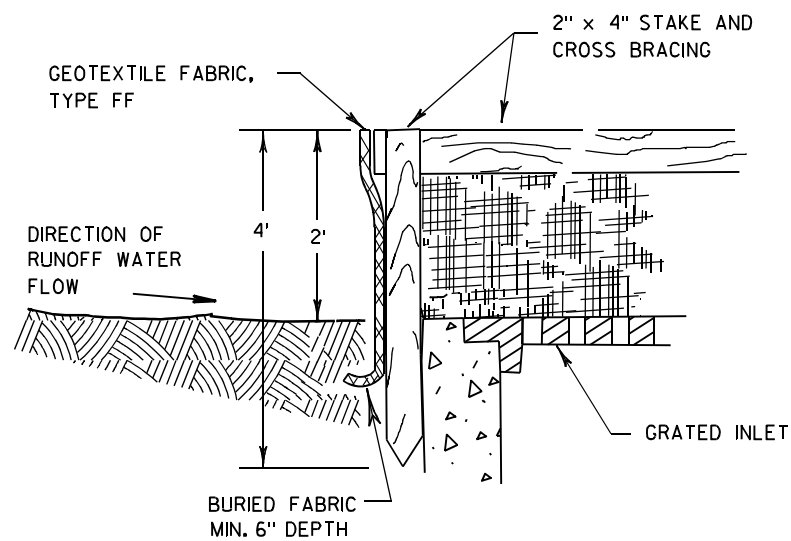
ALL ITEMS ARE CATEGORY 1000 UNLESS OTHERWISE SPECIFIED.



PROJECT NO:1206-04-65	HWY: USH 12	COUNTY: DANE	PLAN SHEET	SHEET	E
-----------------------	-------------	--------------	------------	-------	---

Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C19-02C	MOVING PAVEMENT MARKING OPERATION MULTI -LANE DIVIDED ROADWAY
15C31-01A	PAVEMENT MARKING (RAMPS AND GORES)
15D03-02	TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M. P. H. WITH BARRIER
15D14-03	TRAFFIC CONTROL, TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY, SHORT-TERM (LESS THAN 24 HOURS)
15D15-01	TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE
15D16-02	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D27-02	TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH



INLET PROTECTION, TYPE A

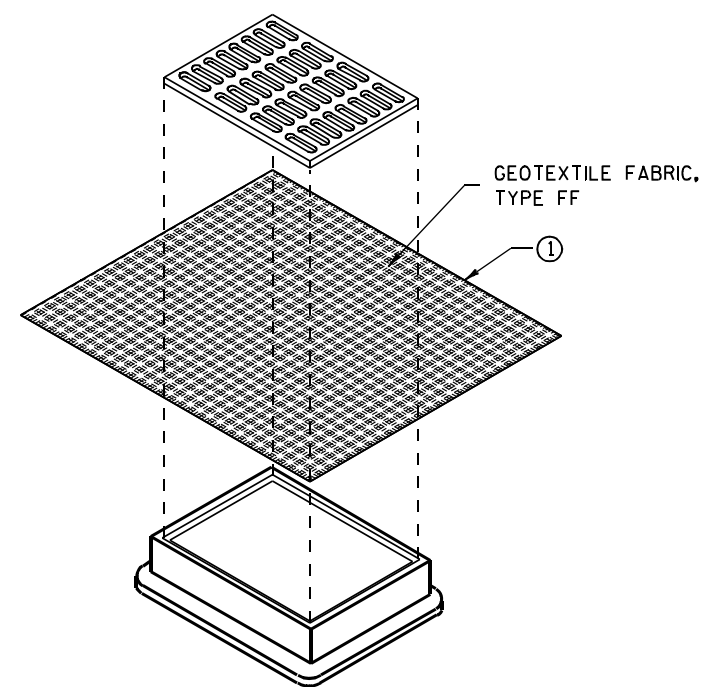
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

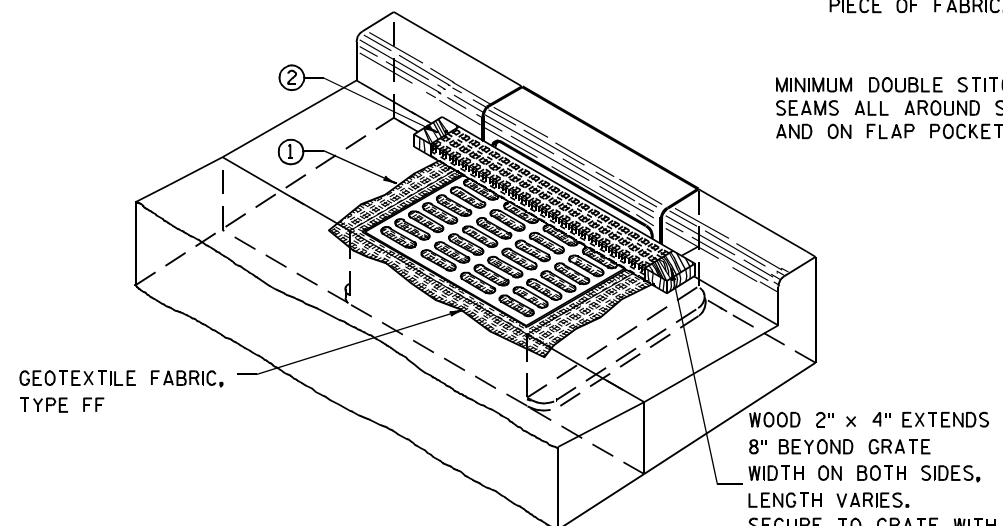
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

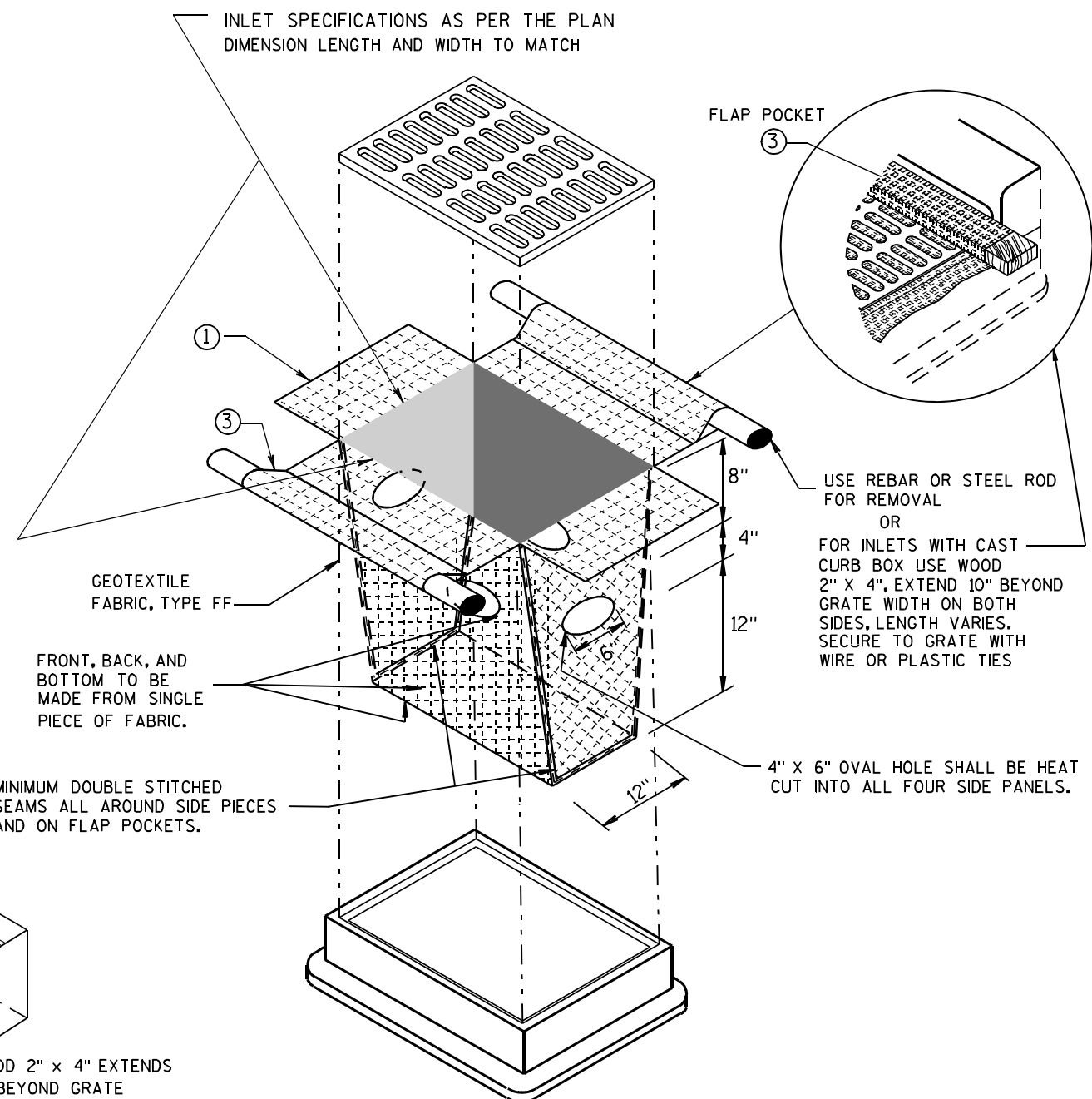
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

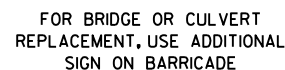
**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Cannestra
DATE
FHWA CHIEF ROADWAY DEVELOPMENT ENGINEER



(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL.
THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

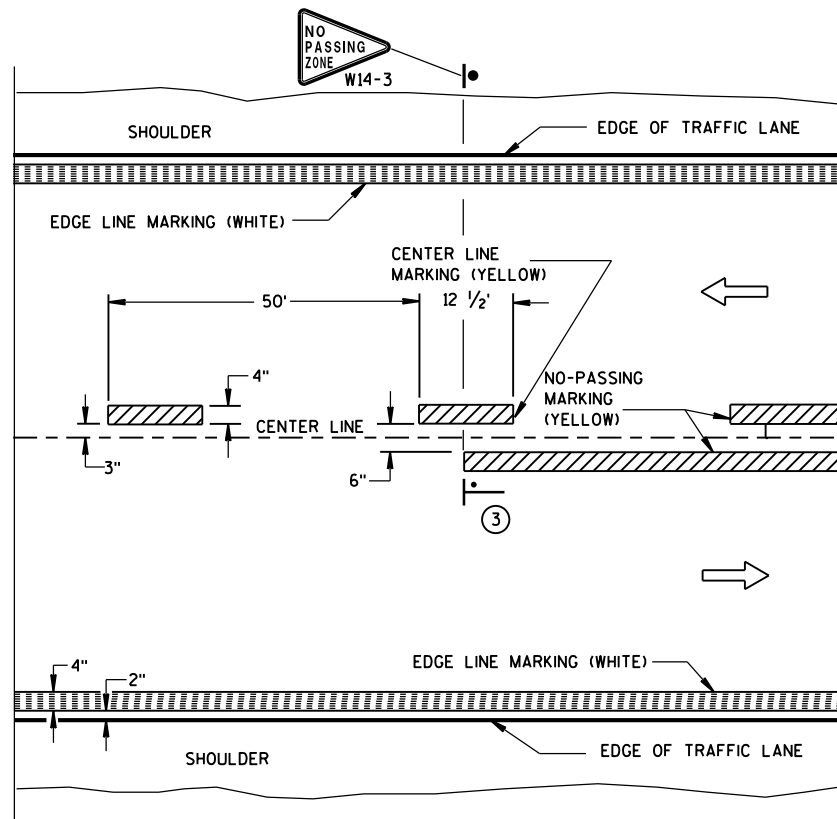
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

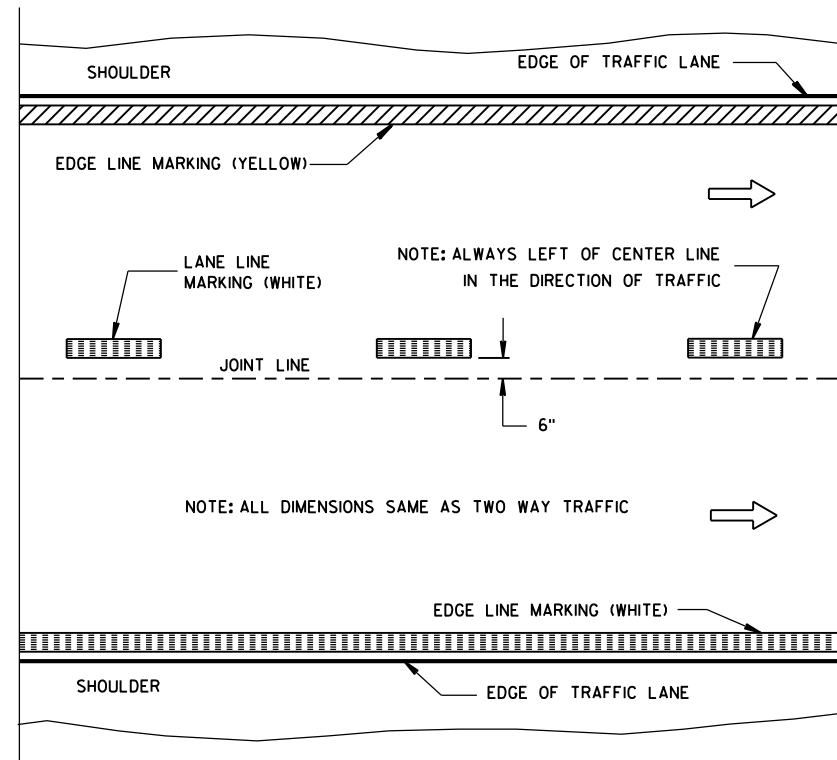
**500' MAX. OR AT LAST INTERSECTION
WHICHEVER IS CLOSER.

	SIGN ON PERMANENT SUPPORT
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TYPE "A" WARNING LIGHT (FLASHING)
	WORK AREA

<p>BARRICADES AND SIGNS FOR SIDEROAD CLOSURES</p>	
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>	
<p>APPROVED 8/2013 DATE</p>	<p><u>/S/ Travis Feltes</u> STATE TRAFFIC ENGINEER OF DESIGN</p>
<p>FHWA</p>	

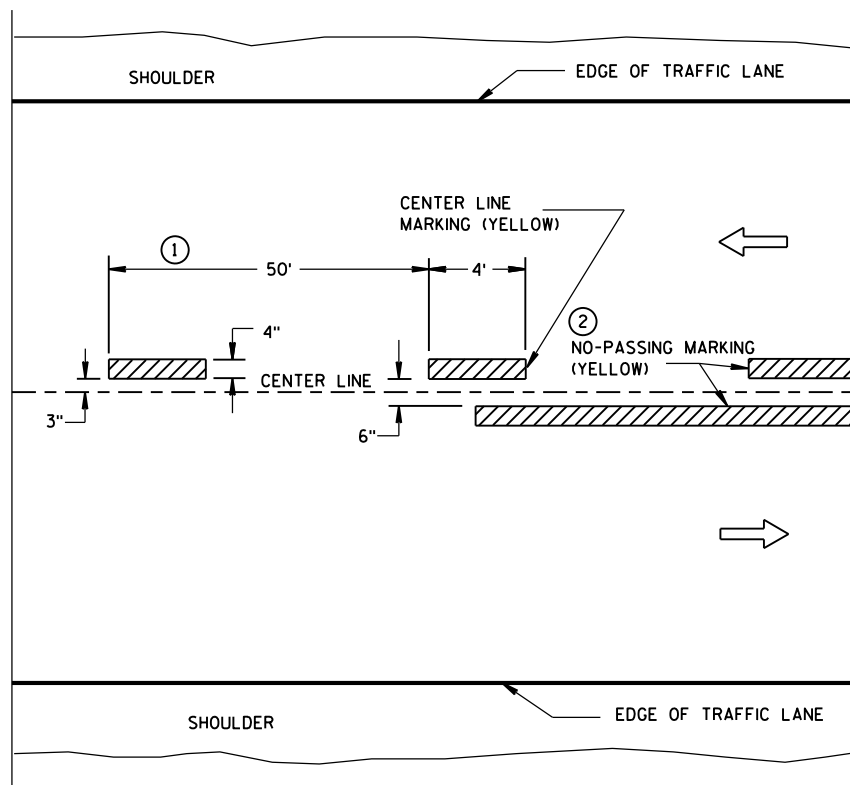


TWO WAY TRAFFIC

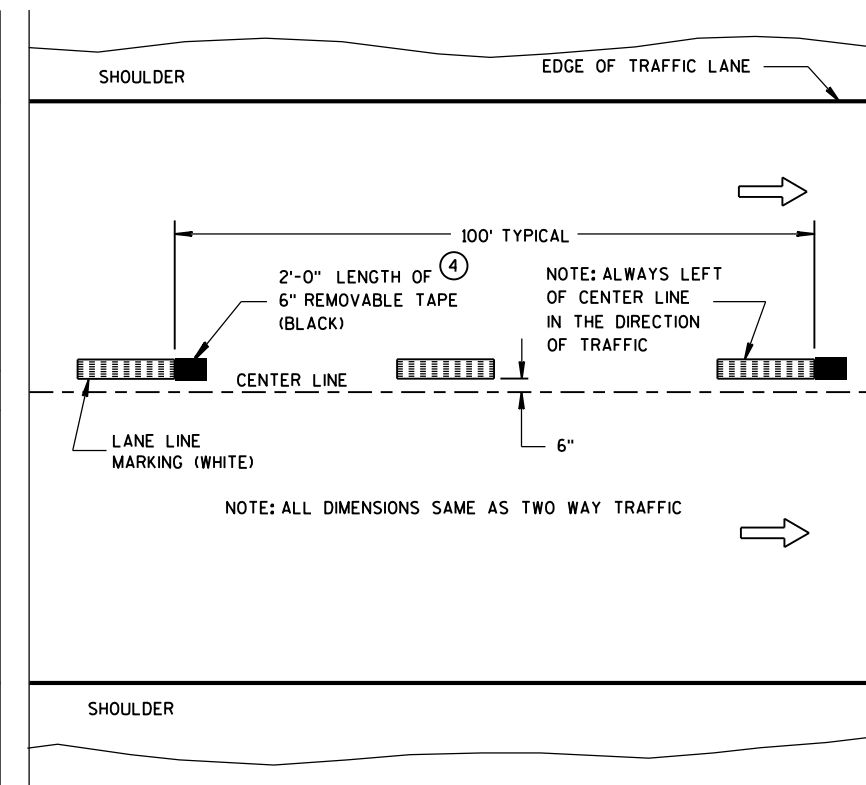


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

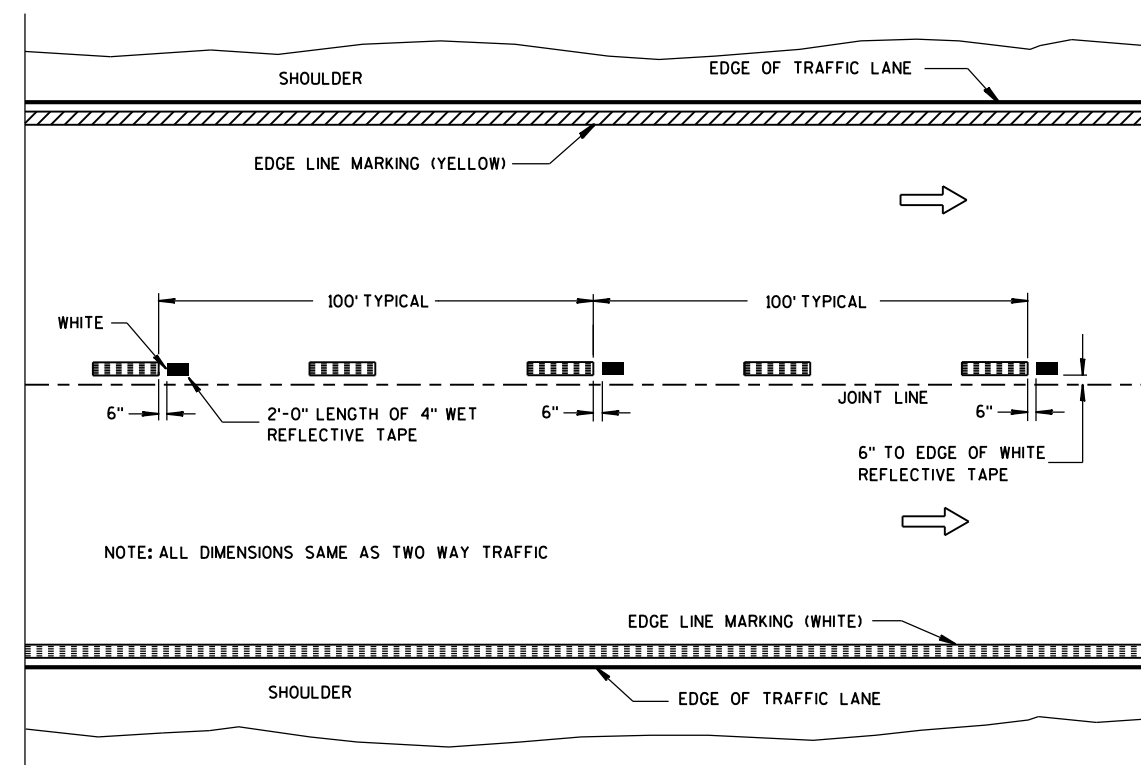
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- ② NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- ③ NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- ④ CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



**WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE**

LEGEND

- "T" MARKING
- POST MOUNTED SIGN





PAVEMENT MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013
DATE
FHWA

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

LEGEND

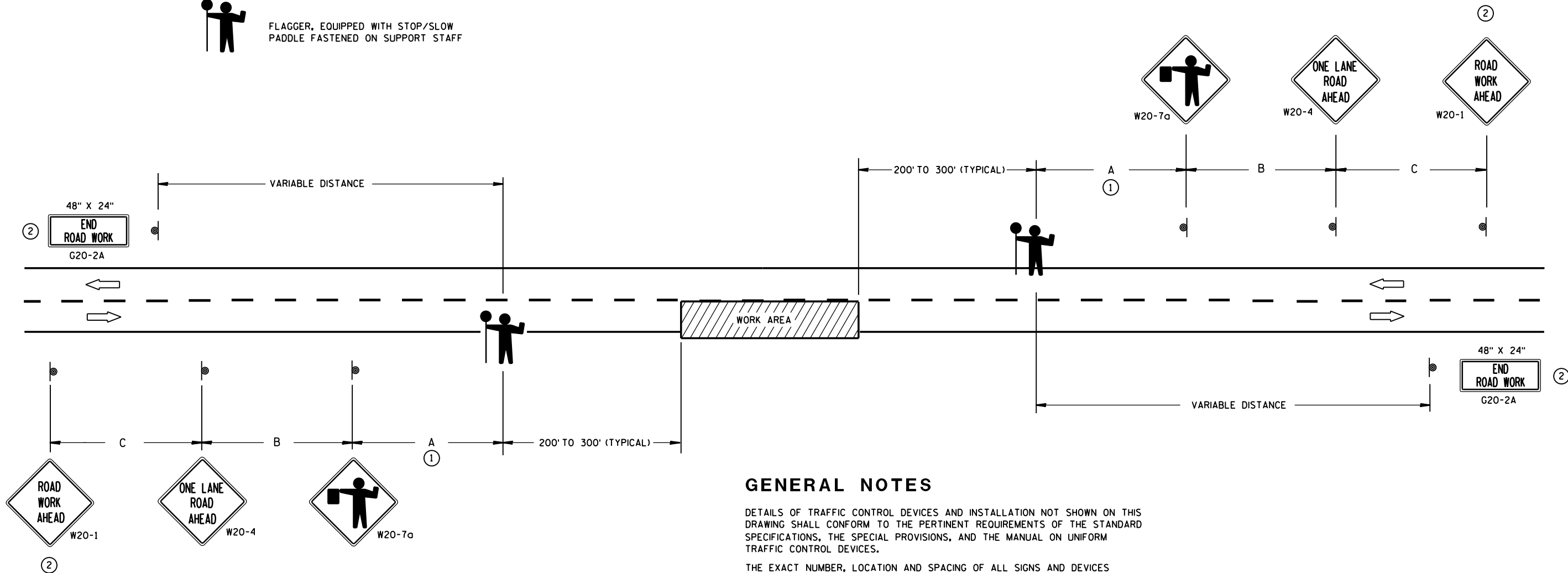
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

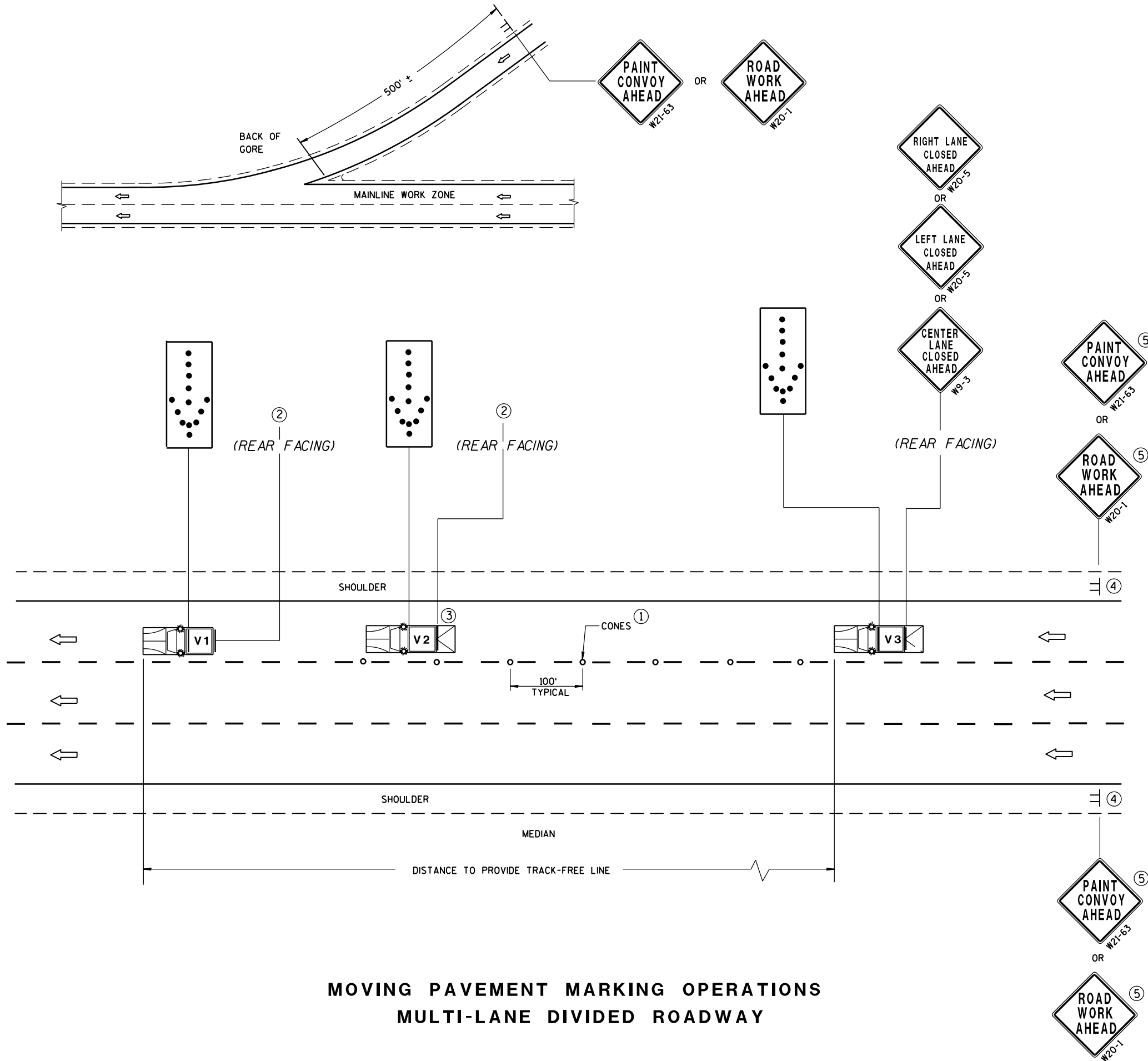
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



MOVING PAVEMENT MARKING OPERATIONS
MULTI-LANE DIVIDED ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

FOR EDGE LINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH. USE DOUBLE ARROWS WHEN CONVOY IS IN CENTER LANE ONLY.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

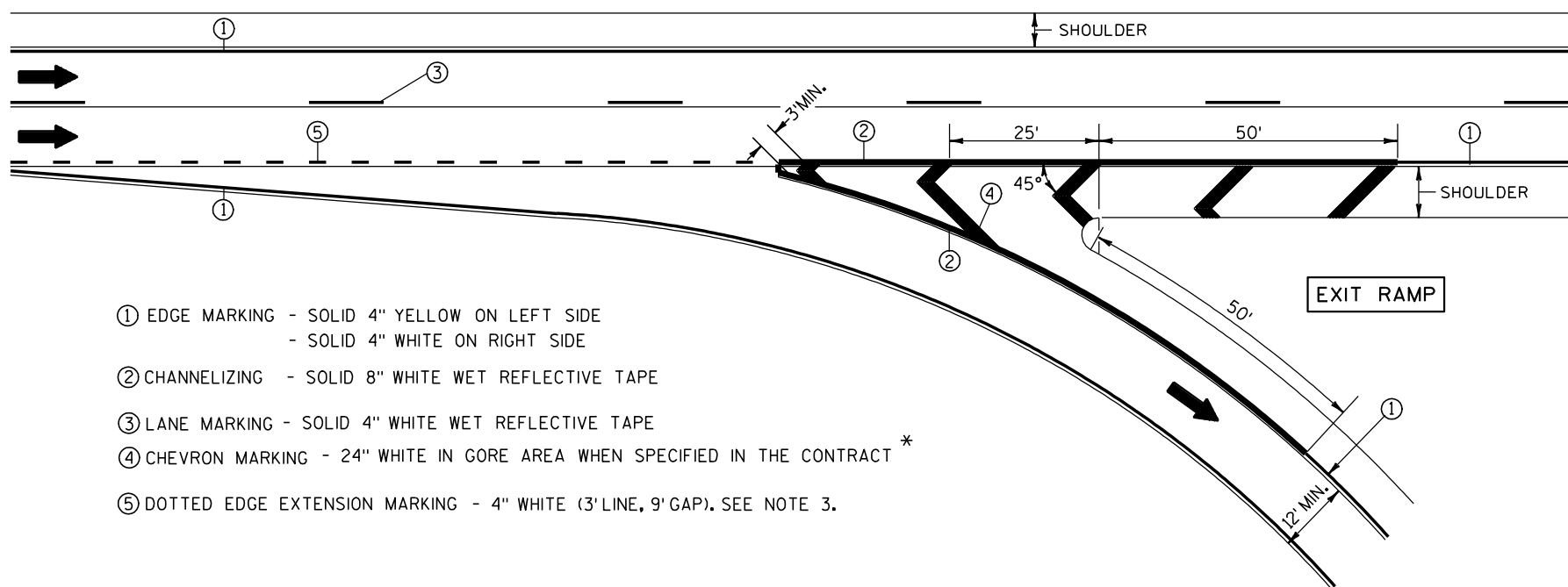
THIS DRAWING SHALL BE USED FOR EDGE LINE OR LANELINE MARKING FOR MULTILANE DIVIDED ROADWAYS.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED AFTER EVERY ON RAMP OR EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

- V1 LEAD VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE WITH TMA
- TMA TRUCK-MOUNTED ATTENUATOR
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- CONES
- FLASHING ARROW PANEL (MERGE)

MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 5/3/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER
FHWA	

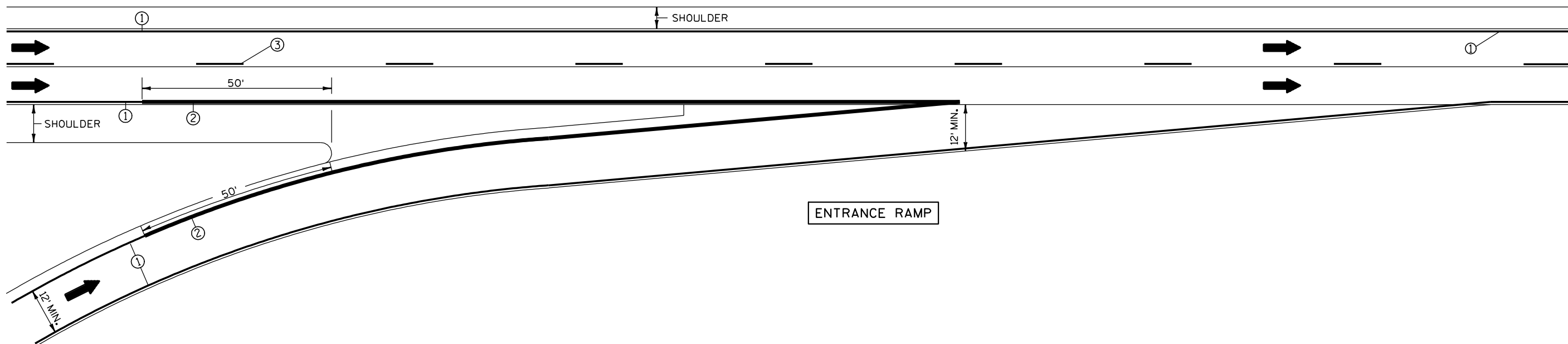


- ① EDGE MARKING - SOLID 4" YELLOW ON LEFT SIDE
- SOLID 4" WHITE ON RIGHT SIDE
- ② CHANNELIZING - SOLID 8" WHITE WET REFLECTIVE TAPE
- ③ LANE MARKING - SOLID 4" WHITE WET REFLECTIVE TAPE
- ④ CHEVRON MARKING - 24" WHITE IN GORE AREA WHEN SPECIFIED IN THE CONTRACT *
- ⑤ DOTTED EDGE EXTENSION MARKING - 4" WHITE (3' LINE, 9' GAP). SEE NOTE 3.

NOTES:

- 1. ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
- 2. PLACE WHITE EDGE OF TAPE 6" LEFT FROM JOINT.
- 3. 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- 4. RETRACE EXISTING DIAGONAL MARKINGS.

* REFER TO DESIGN NOTES.

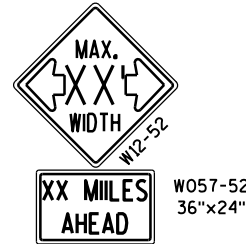


PAVEMENT MARKING
(RAMPS AND GORES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- REMOVING PAVEMENT MARKING
- CONCRETE BARRIER TEMPORARY PRECAST
- DIRECTION OF TRAFFIC
- WORK AREA



INSTALL ON EACH APPROACH AT THE CLOSEST INTERSECTION WITH A STATE OR COUNTY TRUNK HIGHWAY, OR AS DIRECTED BY THE ENGINEER. WIDTH ON SIGN TO BE APPROX. 1 FOOT LESS THAN AVAILABLE WIDTH (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET).



LOCATED 500 FEET IN ADVANCE OF R2-1 SIGN AND 500 FEET BEYOND THE "ROAD WORK 1 MILE" SIGN.



R2-1
48"x60"
(BLACK AND WHITE)

IF THE REGULATORY SPEED HAS BEEN REDUCED, A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. THERE SHOULD BE A SPEED LIMIT SIGN INCORPORATED A MINIMUM OF EVERY 2 OR 3 MILES.

* INCLUDE RESUME SPEED LIMIT SIGN A MINIMUM OF 200 FEET (500 FEET DESIRABLE) AFTER END ROAD WORK SIGNS.

GENERAL NOTES

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

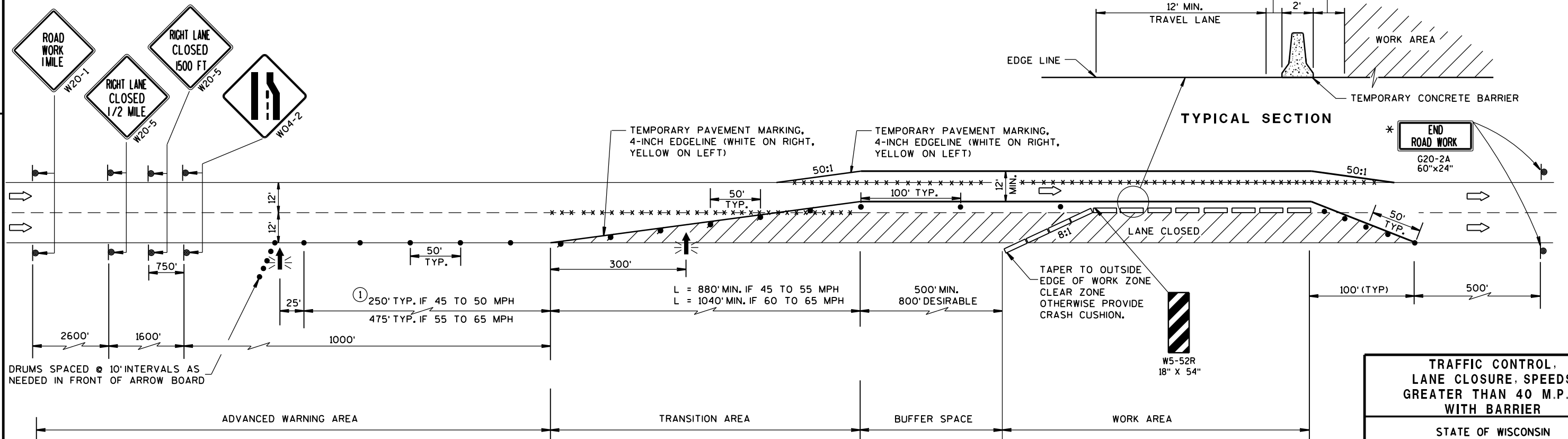
1. CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUM TAPER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

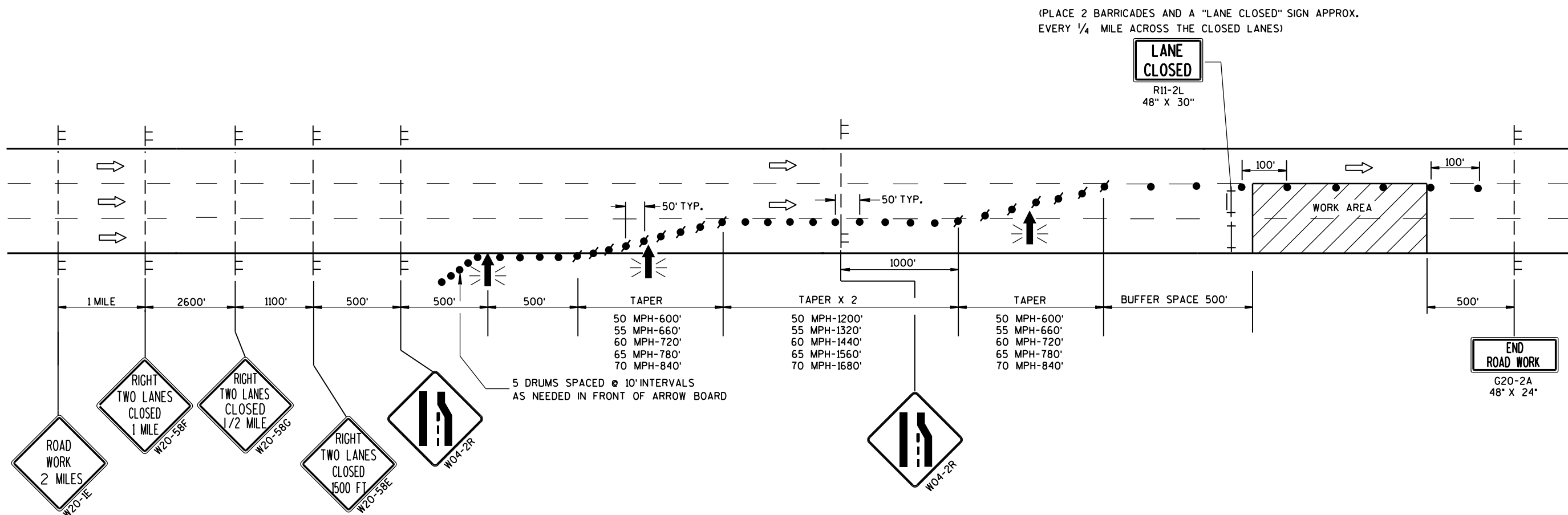


DRUMS SPACED @ 10' INTERVALS AS NEEDED IN FRONT OF ARROW BOARD

TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M.P.H. WITH BARRIER	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013	/S/ Travis Feltes
DATE	STATE TRAFFIC ENGINEER OF DESIGN
FHWA	

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- WORK AREA



GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT TWO LANES. FOR CLOSING THE LEFT TWO LANES, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1E AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

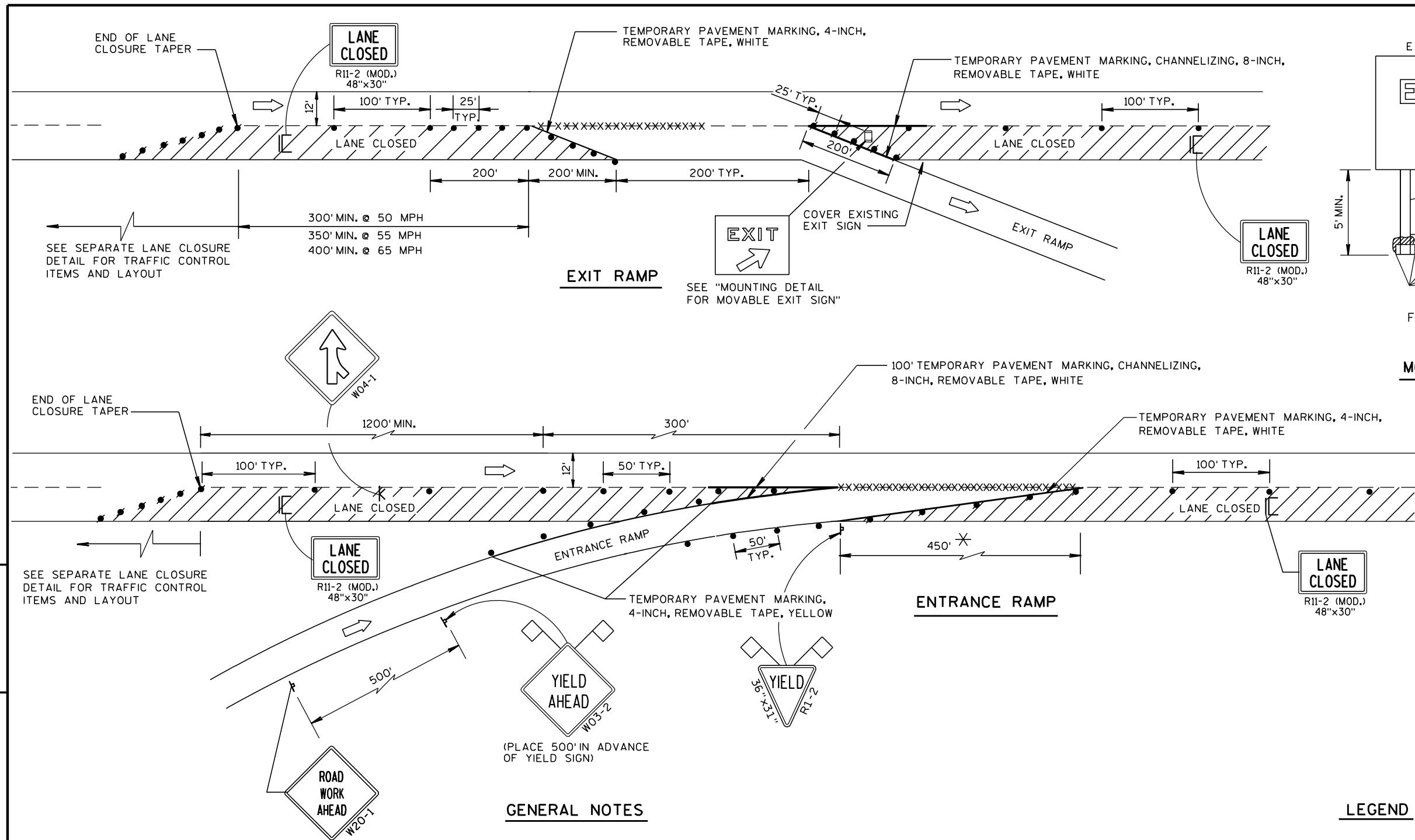
WHEN A RAMP OR SIDE ROAD INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL. TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY. SHORT TERM (LESS THAN 24 HOURS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 14, 2015 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	



GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2 (MOD.) "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

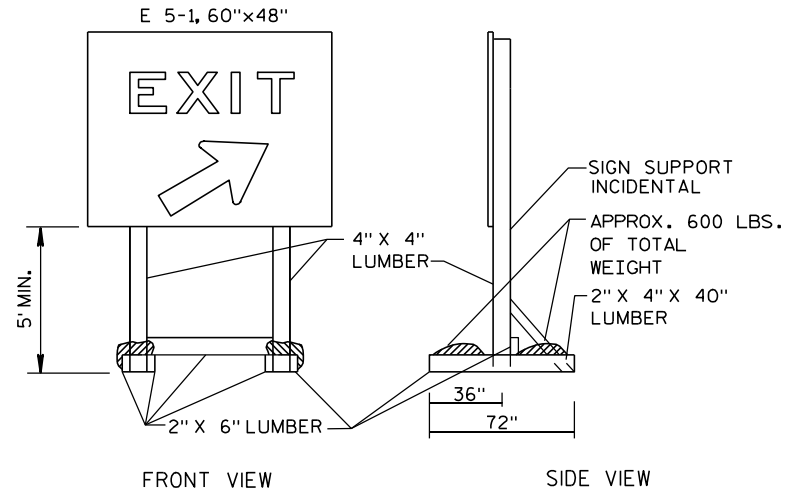
IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 7 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

* LENGTH OF OPENING MAY BE REDUCED TO 150 FEET DURING STAGING OF WORK IN IMMEDIATE AREA OF RAMP TAPER.



NOTE: ALL LUMBER DIMENSIONS ARE NOMINAL
MOUNTING DETAIL FOR MOVABLE EXIT SIGN

LEGEND

- POST MOUNTED SIGN
- SIGN ON PORTABLE SUPPORT
- TRAFFIC CONTROL, DRUM
- TRAFFIC CONTROL, DRUM WITH WARNING LIGHT, TYPE C (STEADY-BURN)
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- TYPE III BARRICADE (8' EQUIVALENT) WITH SIGN
- FLAGS, 16"x16" MIN., ORANGE
- DIRECTION OF TRAFFIC FLOW

TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/24/2000 DATE /S/ Chester J. Spang
CHIEF SIGNS AND MARKING ENGINEER
FHWA

LEGEND

- † TYPE III BARRICADE
- †† TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- Ⓐ TYPE "A" WARNING LIGHT (FLASHING)
- ➡ DIRECTION OF TRAFFIC

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

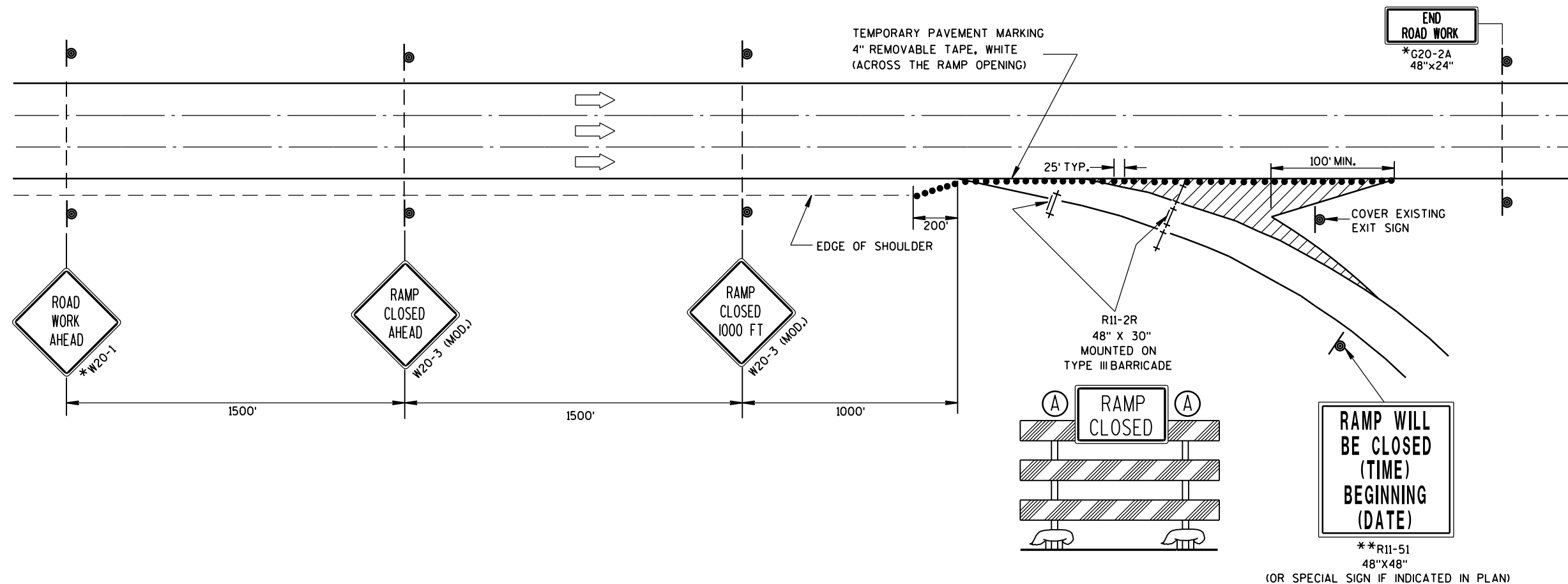
PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF RAMP CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROPOFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN THE WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12-HOUR DURATION.

*W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

** PLACE "RAMP WILL BE CLOSED" SIGN 10 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



TRAFFIC CONTROL,
EXIT RAMP CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ⚡➡ FLASHING ARROW BOARD
- ▨ WORK AREA

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

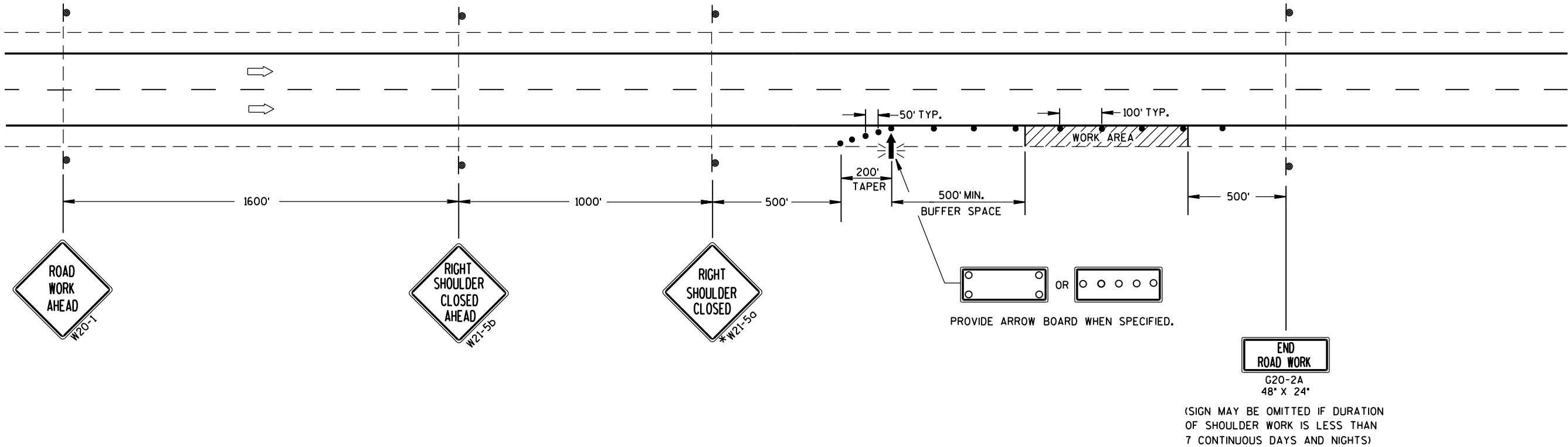
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-5a SIGN MAY BE OMITTED.



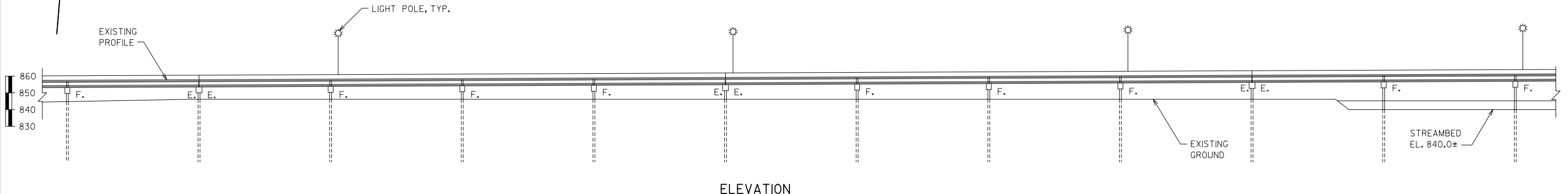
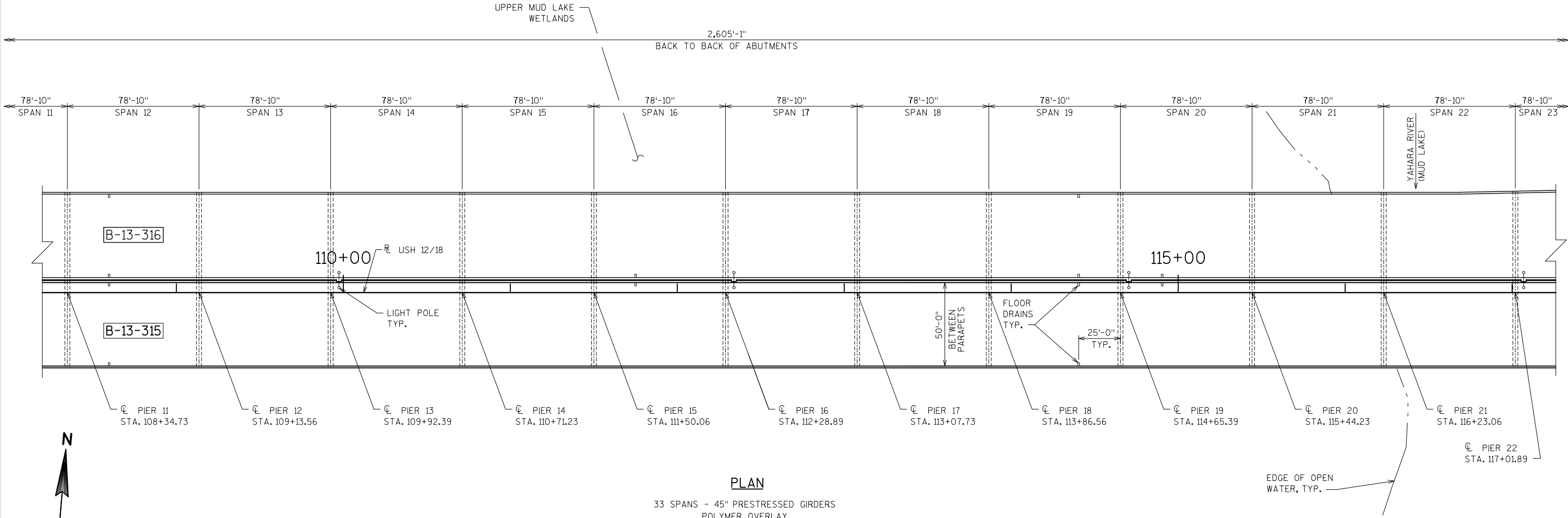
TRAFFIC CONTROL
SHOULDER CLOSURE ON DIVIDED
ROADWAY, SPEEDS GREATER
THAN 40 MPH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

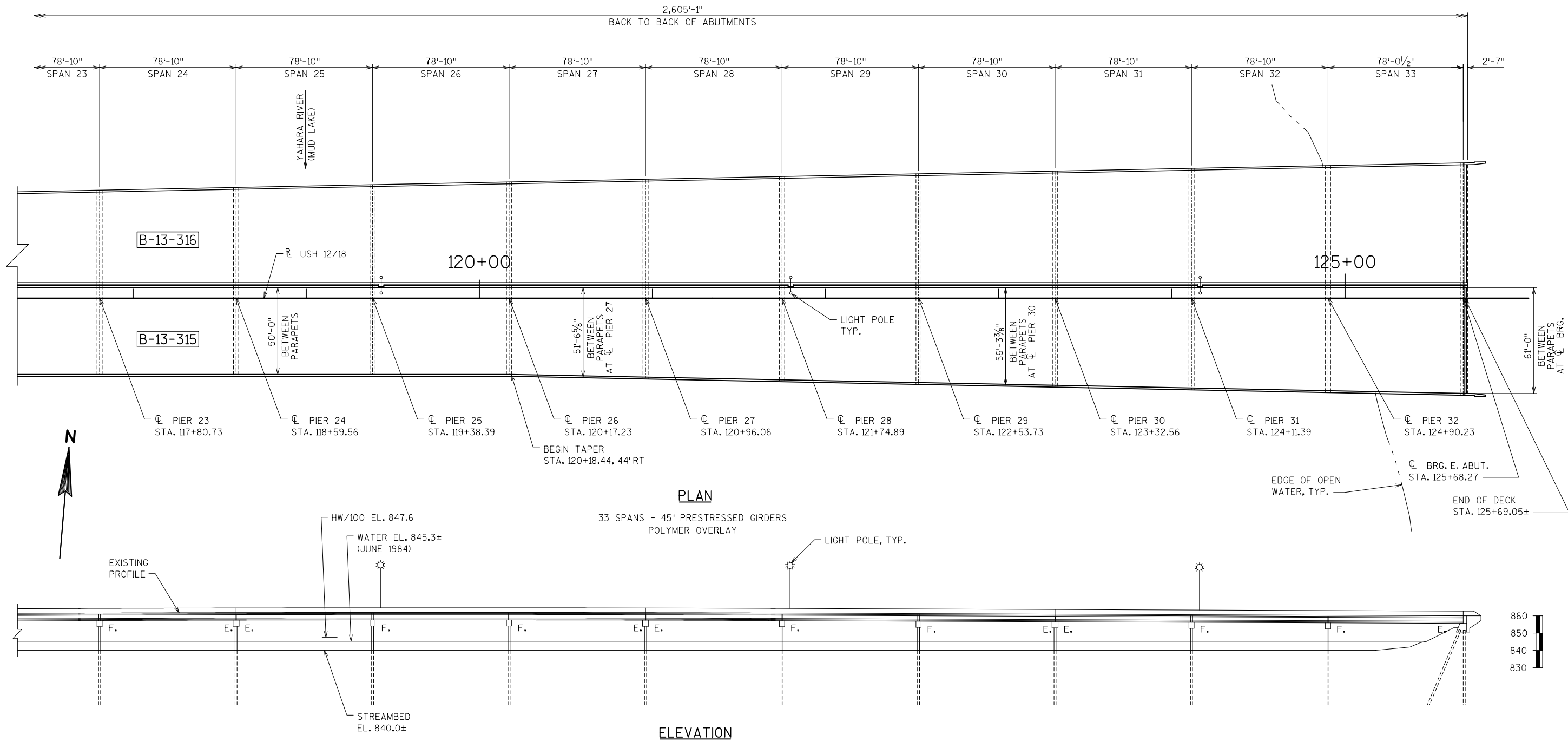
APPROVED
8/2013 /S/ Travis Feltz
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



NO.	DATE	REVISION	BY
		1555 N RiverCenter Drive Suite 214 Milwaukee, WI 53212 (414) 944-6080	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
ACCEPTED <u>William C. Decker</u> SDR		02/17/16	
CHIEF STRUCTURES DESIGN ENGINEER		DATE	
STRUCTURE B-13-315			
EB USH 12/18 (BELTLINE) OVER YAHARA RIVER			
COUNTY DANE		TOWN/CITY/VILLAGE MONONA	
DESIGN SPEC.		REHABILITATION N/A	
DESIGNED BY JDB	DESIGN CK'D. MAD	DRAWN BY JDB/DNJ	PLANS CK'D. MAD
GENERAL PLAN 1 OF 3		SHEET 1 OF 4	



NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-13-315			
DRAWN BY		JDB	PLANS CK'D. MAD
GENERAL PLAN 2 OF 3			SHEET 2 OF 4

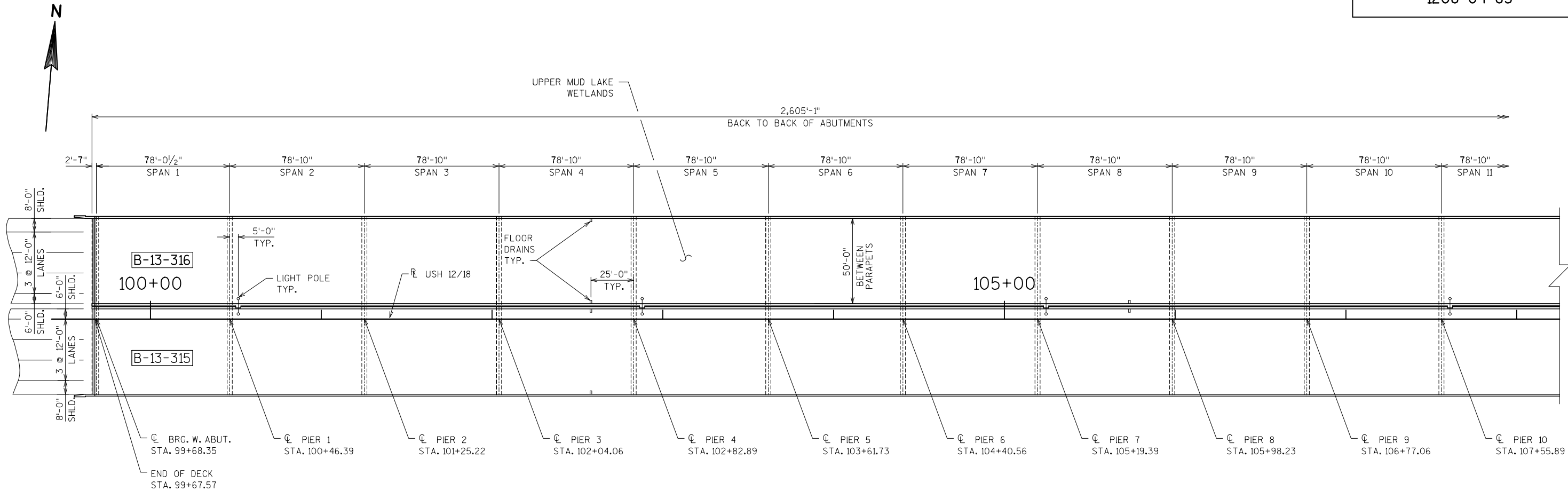


NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-13-315			
DRAWN BY		JDB	PLANS CK'D. MAD
GENERAL PLAN 3 OF 3			SHEET 3 OF 4



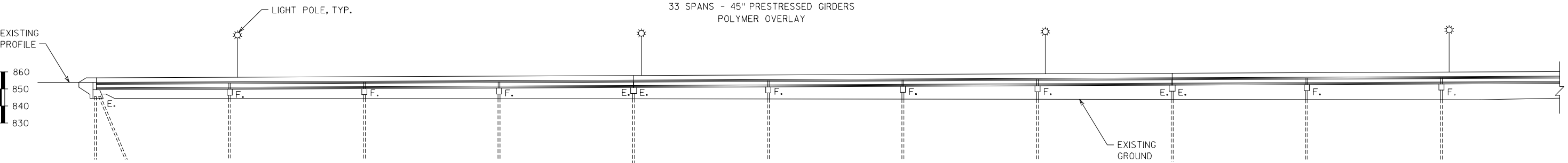
BID ITEM #	BID ITEM	UNIT	SUPER- STRUCTURE
509.0301	PREPARATION DECKS TYPE 1	SY	9
509.0302	PREPARATION DECKS TYPE 2	SY	4
SPV.0025.01	POLYESTER POLYMER CONCRETE MASONRY	CF	19
SPV.0090.01	SAWING PAVEMENT DECK PREPARATION AREAS	LF	90
SPV.0180.01	RAPID CURE POLYMER OVERLAY	SY	14,806

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-13-315			
		DRAWN BY JDB/DNJ	PLANS CK'D. MAD
CROSS SECTION & QUANTITIES		SHEET 4 OF 4	



PLAN

33 SPANS - 45" PRESTRESSED GIRDERS
POLYMER OVERLAY



ELEVATION

DESIGN DATA

LIVE LOAD: HS20
DESIGN LOADING: HS14
INVENTORY RATING: HS24
OPERATING RATING: HS24
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV) = 190 KIPS

HYDRAULIC DATA

(TAKEN FROM HSI, 08/28/2014)
100 YEAR FREQUENCY:
Q / 100 = 600 CFS
VELOCITY = 0.2 FPS
HW ELEV. = 847.6
WATERWAY AREA = 9420 SQ. FT.
DRAINAGE AREA = 281 SQ. MI.
OVERTOPPING RD = N/A

TRAFFIC VOLUME

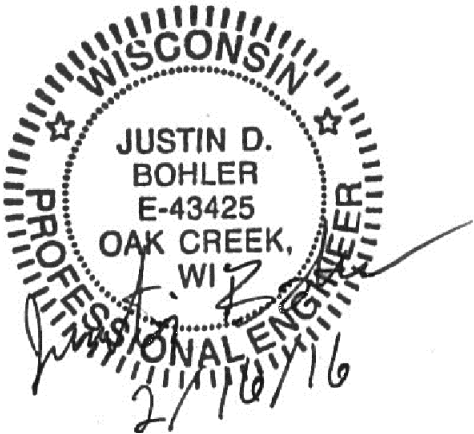
USH 12 & 18
A.D.T = 56,100 (2006)
R.D.S. = 60 M.P.H.

GENERAL NOTES

THE PROPOSED WORK INCLUDES PLACEMENT OF A POLYMER OVERLAY.
DRAWINGS SHALL NOT BE SCALED.
DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
ENTIRE DECK SURFACE SHALL BE PREPARED TO RECEIVE A POLYMER OVERLAY BY SHOTBLASTING. REFER TO THE PROJECT SPECIFICATIONS FOR DETAILS ON REQUIREMENTS FOR DECK PREPARATION. SHOTBLASTING DECK PREPARATION IS INCIDENTAL TO THE POLYMER OVERLAY BID ITEM.

LIST OF DRAWINGS

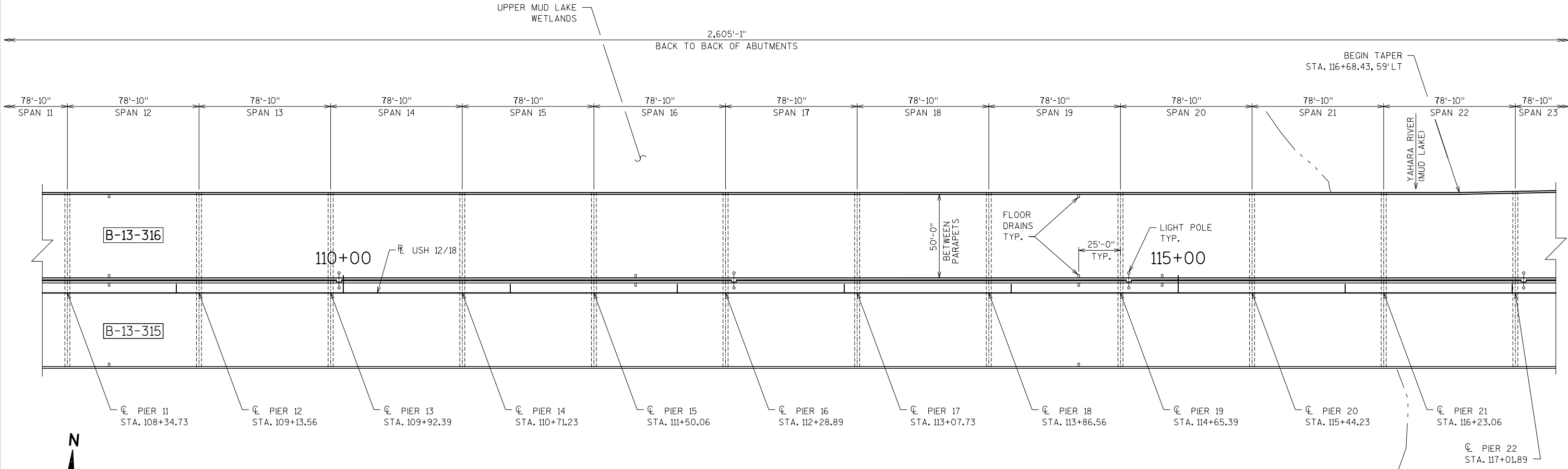
- GENERAL PLAN 1 OF 3
- GENERAL PLAN 2 OF 3
- GENERAL PLAN 3 OF 3
- CROSS SECTION & QUANTITIES



STRUCTURE DESIGN CONTACTS

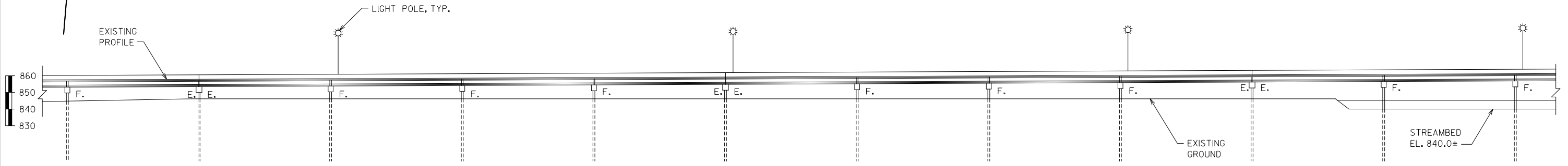
CONSULTANT: MICHAEL DELEMONT 414-944-6200
BRIDGE OFFICE: WILLIAM DREHER 608-266-8489

NO.	DATE	REVISION	BY
AECOM		1555 N RiverCenter Drive Suite 214 Milwaukee, WI 53212 (414) 944-6080	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
ACCEPTED	<i>William C. Dreher</i> SDR		02/17/16
CHIEF STRUCTURES DESIGN ENGINEER			DATE
STRUCTURE B-13-316			
WB USH 12/18 (BELTLINE) OVER YAHARA RIVER			
COUNTY	DANE	TOWN/CITY/VILLAGE	MONONA
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	JDB	DESIGN CK'D.	MAD
DRAWN BY	JDB/DNJ	PLANS CK'D.	MAD
GENERAL PLAN 1 OF 3			SHEET 1 OF 4



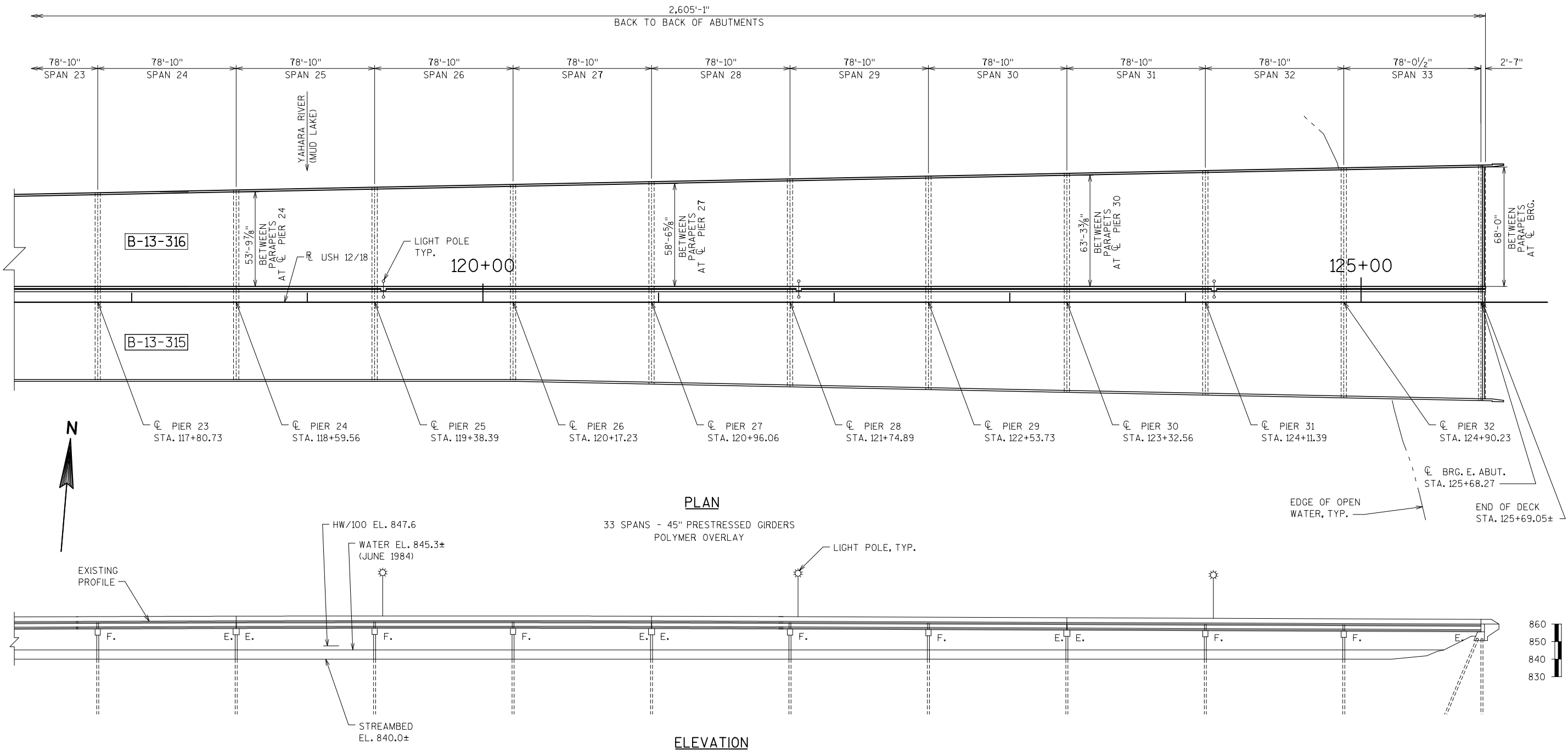
PLAN

33 SPANS - 45" PRESTRESSED GIRDERS
POLYMER OVERLAY

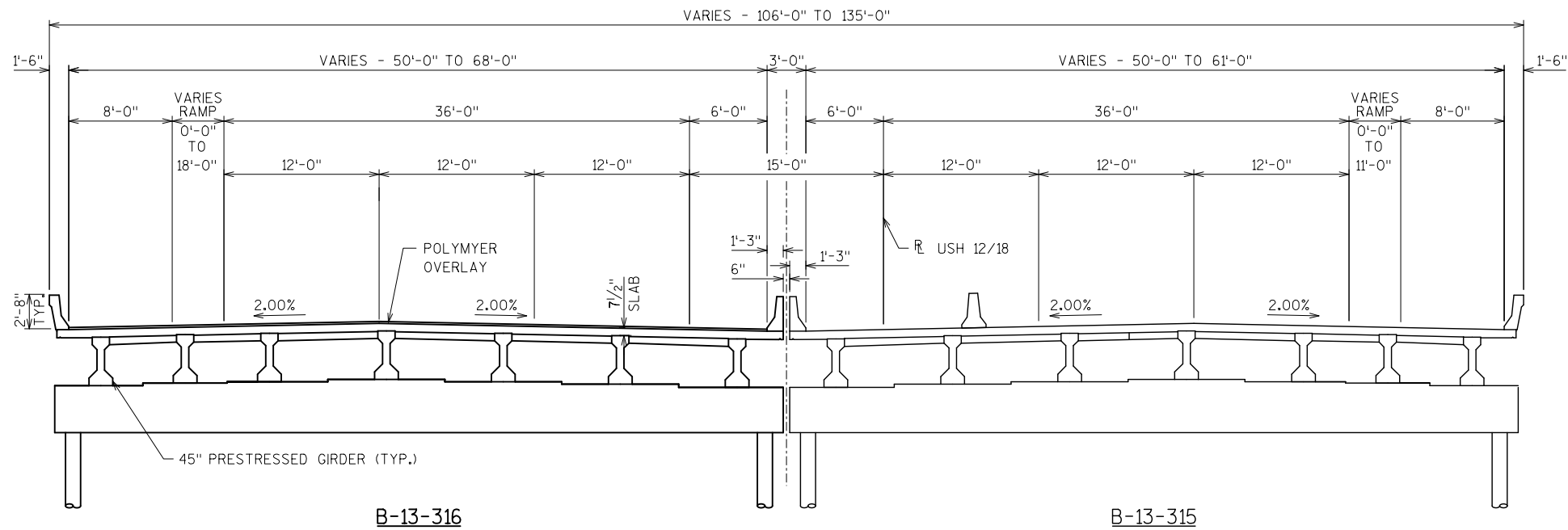


ELEVATION

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-13-316			
		DRAWN BY	JDB PLANS CK'D. MAD
GENERAL PLAN 2 OF 3		SHEET 2 OF 4	



NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-13-316			
DRAWN BY		JDB	PLANS CK'D. MAD
GENERAL PLAN 3 OF 3		SHEET 3 OF 4	



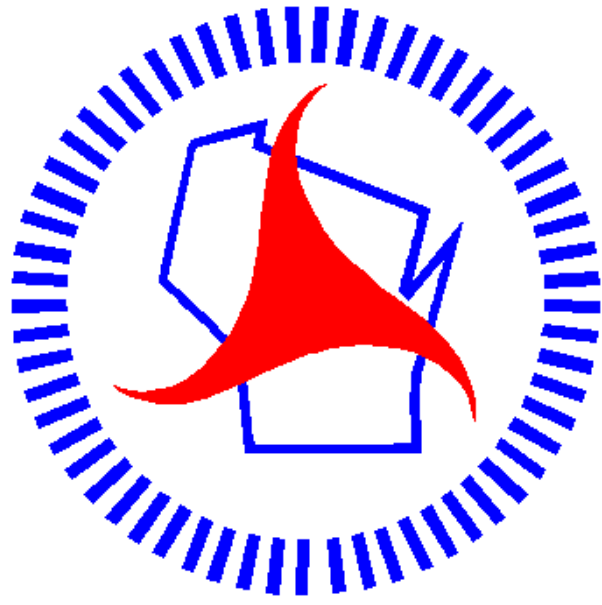
CROSS SECTION THRU ROADWAY
(LOOKING EAST)

TOTAL ESTIMATED QUANTITIES

BID ITEM #	BID ITEM	UNIT	SUPER-STRUCTURE
509.0301	PREPARATION DECKS TYPE 1	SY	9
509.0302	PREPARATION DECKS TYPE 2	SY	4
SPV.0025.01	POLYESTER POLYMER CONCRETE MASONRY	CF	19
SPV.0090.01	SAWING PAVEMENT DECK PREPARATION AREAS	LF	90
SPV.0180.01	RAPID CURE POLYMER OVERLAY	SY	15,371

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-13-316			
DRAWN BY JDB/DNJ		PLANS CK'D.	MAD
CROSS SECTION & QUANTITIES			SHEET 4 OF 4

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>