April 2016

ORDER OF SHEETS

Section No. 1 Section No. 2 Typical Sections and Details Section No. 3 Estimate of Quantities Section No. 3 Miscellaneous Quantities Right of Way Plat Section No. 5 Plan and Profile Section No. 6 Standard Detail Drawings Section No. 7 Sign Plates Section No. 8 Structure Plans Section No. 9 — Computer Earthwork Data

TOTAL SHEETS = 166

Section No. 9 Cross Sections

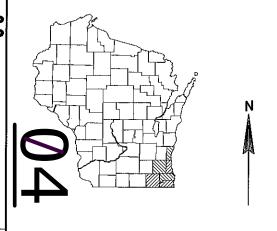
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

FEDERAL PROJECT STATE PROJECT CONTRACT 1000-20-68

SIGN BRIDGE REPAIR AND REPLACEMENT 2016

VARIOUS FREEWAYS, STATE HIGHWAYS, AND US HIGHWAYS SOUTHEAST REGION WIDE

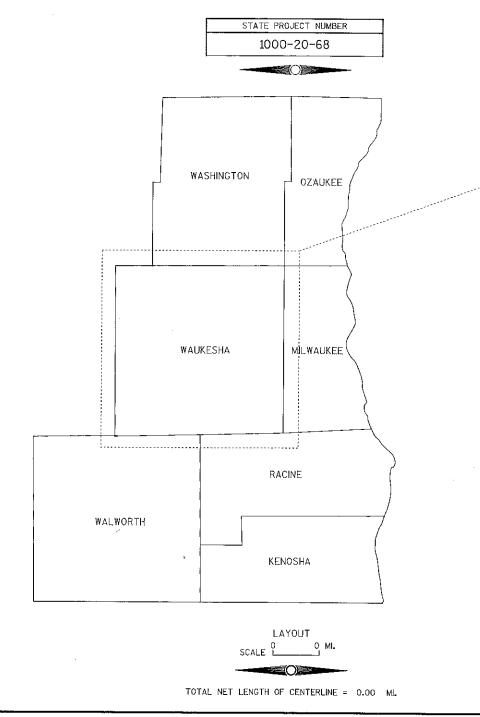


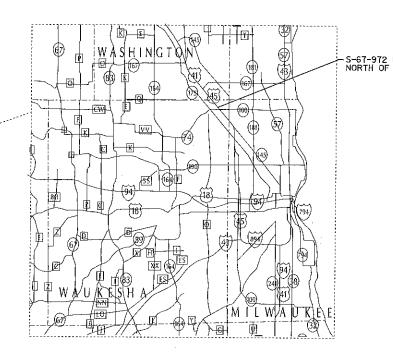
DESIGN DESIGNATION N/A

A.D.T.	Ξ	N/A
A.D.T.	=	N/A
D.H.V.	=	N//
D.	=	N//
T.	=	N/A
DESIGN SPEED	=	N//
ESALS	=	N/#

CONVENTIONAL SYMBOLS

CONTENTIONAL STRIBULS			
PLAN .		PROFILE	ea.
CORPORATE LIMITS	<u>///////</u>	GRADE LINE	8
PROPERTY LINE		ORIGINAL GROUND MARSH OR ROCK PROFILE	 ROCK
LOT LINE		(To be noted as such)	
LIMITED HIGHWAY EASEMENT	L	SPECIAL DITCH	LABEL
EXISTING RIGHT OF WAY PROPOSED OR NEW R/W LINE		GRADE ELEVATION	95.36
SLOPE INTERCEPT		CULVERT (Profile View)	\Diamond \square
REFERENCE LINE	-	UTILITIES	
EXISTING CULVERT		ELECTRIC	—— E ——
PROPOSED CULVERT		FIBER OPTIC	F0
(Box or Pipe)		GAS	G
COMBUSTIBLE FLUIDS	M	SANITARY SEWER	
COMBOSTIBLE PEOPS	-CAUTION-	STORM SEWER	ss
	V	TELEPHONE	— т —
MARSH AREA	(I * I)	WATER	— w —
THE STREET PROPERTY.	* * _ * _ /	UTILITY PEDESTAL	Ħ
	eccentrication .	POWER POLE	ⅎ





DEPARTMENT OF TRANSPORTATION REPARED BY Surveyor COLLINS ENGINEERS, INC. Designer Project Manager

STATE OF WISCONSIN

WOODED OR SHRUB AREA FILE NAME: 00_Title Sheet.dgn

PLOT BY: Veronica Chavezv8I

TELEPHONE POLE

Pdf_bw_HS_by level,plt

CEILTextSub_ScreenControlByLeveI,tbl

PLOT SCALE: \$\$.....plotscale.....\$\$ WISDOT/CADDS SHEET 10

UTILITIES CONTACT LIST

MILWAUKEE COUNTY HIGHWAY DIVISION ELECTRICAL SHOP MR. STANLEY JACKSON 10320 W. WATERTOWN PLANK RD. WAUWATOSA, WI. 53226 (414) 257-6593

WE ENERGIES (GAS) MR. TOM MINESAL 5400 GREEN BAY AVE. MILWAUKEE, WI 53209 414-944-5755

WE ENERGIES-ELECTRIC MR. DAN SANDE 333 W EVERETT ST.-A279 MILWAUKEE WI 53203 414-221-4578 DAN.SANDE@WE-ENERGIES.COM WISCONSIN DEPARTMENT OF TRANSPORTATION SE REGION LIGHTING ENGINEER MR. MATT PFEIFER 262-548-8778 MATTHEW.PFEIFER@DOT.GOV

WISCONSIN DEPARTMENT OF TRANSPORTATION TRAFFIC OPERATIONS CENTER MR. JEFF MADISON 433 W ST PAUL AVE, STE 300 MILWAUKEE, WI 53203 414-255-3723 JEFFERY.MADISON@DOT.WI.GOV

DNR CONTACT LIST

MILWAUKEE AND RACINE COUNTY MS. KRISTINA BETZOLD 2300 N. MARTIN LUTHER KING DR. MILWAUKEE, WI 53212 (414) 263-8517

WASHINGTON COUNTY MR. MIKE THOMPSON 2300 N. MARTIN LUTHER KING DR. MILWAUKEE, WI 53212 (414) 263-8648

KENOSHA AND WAUKESHA COUNTY MR. CRAIG WEBSTER 141 N W BARSTOW ST., RM. 180 WAUKESHA, WI 53187 (262) 574-2141

DESIGN CONTACT

COLLINS ENGINEERS, INC. 2033 W. HOWARD AVE MILWAUKEE, WI 53221 ATTN: VERONICA CHAVEZ DE FERNANDEZ (414) 282-6905

GENERAL NOTES

THE LOCATION OF EXISTING OR PROPOSED UTILITIES AS NOTED ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. UTILITY SERVICES ARE NOT SHOWN.



PROJECT NO: 1000-20-68 HWY: VARIOUS

COUNTY: SOUTHEAST REGION WIDE

CEI_TextSub_ScreenControlByLevel.tbl

GENERAL NOTES AND UTILITY CONTACTS

Ε

PLOT DATE: 11/24/2015

FILE NAME: 02_01_Utility.dgn

Pdf_bw_HS_by level.plt

PLOT NAME :

SIGN BRIDGE LOCATIONS - KENOSHA COUNTY

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-30-025	l-94	NB	AT EXIT TO WEIGH STATION
S-30-035	l-94	SB	AT EXIT TO RUSSEL RD
S-30-232	STH 165	EB	AT CTH ML
S-30-233	STH 165 WB	WB	AT CTH ML
S-30-234	STH 165	WB	AT CTH EZ
S-30-235	STH 165 WB	WB	AT CTH EZ
S-30-236	STH 165	WB	APPROACHING 47TH AVE AFTER ML
S-30-250	STH 31	SB	AT STH 50
S-30-255	WEIGH STATION ENTRANCE RD	NB	WEIGH STATION ENTRANCE RD
S-30-608	COUNTY ML	NB	AT STH 165
S-30-609	COUNTY ML	SB	AT STH 165
S-30-610	COUNTY EZ	SB	AT STH 165
S-30-611	COUNTY EZ	SB	AT STH 165
S-30-612	WEST FRONTAGE RD	SB	AT 71ST ST EAST OF I- 94 FRONTAGE RD

SIGN BRIDGE LOCATIONS - OZAUKEE COUNTY

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-45-001	l-43	SB	AT EXIT TO STH 33
S-45-002	l-43	NB	1/2 MILE SOUTH OF STH 57
S-45-003	I-43	SB	1/2 MILE NORTH OF STH 33 EXIT
S-45-004	l-43	NB	SOUTH OF STH 57/I 43 SPLIT
S-45-201	STH 57 EB	EB	AT I- 43 RAMP
S-45-600	PORT WASHINGTON RD	SB	AT MEQUON RD

SIGN BRIDGE LOCATIONS - RACINE COUNTY

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-51-008	STH 38-NORTHWESTERN	WB	RAPIDS DR ON STH 38
S-51-009	STH 38 NB-NORTHWEST AVE	WB	JUST EAST OF RAPIDS DR
S-51-010	STH 38 SB-NORTHWEST AVE	EB	JUST EAST OF RAPIDS DR
S-51-011	STH 38 SB-NORTHWEST	EB	1/4 WEST OF CTH MM
S-51-214	STH 20	WB	AT RENAISSSANCE BLVD

SIGN BRIDGE LOCATIONS - WASHINGTON COUNTY

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-66-201	PARADISE DR EB	EB	RAMPS AT USH 45

SIGN BRIDGE LOCATIONS - MILWAUKEE COUNTY

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-40-009	l-894	WB	AT 60TH STREET EXIT
S-40-018	l-894	SB	I-894 @ THE HALE INTERCHANGE
S-40-049	STH 100	NB	SOUTH OF USH 45 RAMP
S-40-120	I-43	NB	HALE INT SPLIT WITH USH 45
S-40-126	l-894	SB	EXIT TO OKLAHOMA
S-40-151	I-94	WB	AT EXIT TO HAWLEY RD
S-40-185	STH 145 SB	EB	AT 14TH ST & WALNUT ST
S-40-192	LINCOLN MEMORIAL DR	NB	AT I-794
S-40-193	LINCOLN MEMORIAL DR	SB	AT MICHIGAN ST
S-40-204	I-94	EB	AT 41ST ST
S-40-228	I-43	SB	AT CAPITOL DR
S-40-229	I-43	NB	AT C & NW RAILROAD
S-40-268	HIGHWAY	EB	APPROACHING LAKE PKWY - STH 794
S-40-269	HOWARD AVE	WB	APPROACHING LAKE PKWY - STH 794
S-40-283	STH 32 SB	SB	COLUMBIA
S-40-285	STH 145 WB	WB	APPROACHING HILLAISW INTERCHANGE
S-40-301	STH 794 S	SB	NORTH OF E RUSSEL AVE
S-40-306	GOOD HOPE RD	WB	GOOD HOPE
S-40-326	USH 41 SB-STH 175 SB	SB	AT USH 41
S-40-327	USH 41	NB	AT MILL RD
S-40-328	STH 145 NB	NB	AT PARK PLACE
S-40-352	USH 45	SB	AT HAMPTON OFF RAMP
S-40-355	USH 45	SB	AT NORTH AVE WEST
S-40-393	I-43	SB	AT EXIT TO LAPHAM/MITCHELL ST
S-40-423	GOOD HOPE RD	EB	AT USH 41/45 ON RAMP, PRIOR TO RAMP
S-40-425	HIGHWAY	EB	BEFORE STH 100
S-40-426	STH 175 SB	SB	BEFORE HWY 41
S-40-436	l-894	EB	BLANK OUT SIGNS AT TUNNEL - X & DOWN ARROW
S-40-437	l-894	EB	BLANK OUT SIGNS IN ADV OF TUNNEL - X & DOWN ARROW
S-40-438	I-94	NB	S-W CONNECTOR BLANK OUT SIGNS AT TUNNEL
S-40-443	I-94	WB	EAST OF 68TH ST

SIGN BRIDGE LOCATIONS - WALWORTH COUNTY

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-64-012	USH 12	WB	AT COUNTY NN
S-64-017	USH 12	SB	ON C-D ROAD TO I- 43 NB

SIGN BRIDGE LOCATIONS - MILWAUKEE COUNTY (CONT.)

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-40-502	l-43	NB	SOUTH OF STH 145 EXIT
S-40-503	l-43	NB	OFF RAMP TO MCKINLEY AVE
S-40-512	l-43	SB	1/4 MILE NORHT OF I-94
S-40-521	l-43	NB	S-E CONNECTOR TO 794 - FOR PLANKINTON
S-40-525	I-94	EB	1/10 MILE EAST OF 25TH ST
S-40-531	794	EB	EXIT TO ST. PAUL AVE - JAMES LOVELL ST
S-40-537	I-794	EB	PLANKINTON AVE EXIT TO JACKSON ST
S-40-549	l-43	NB	1/2 MILE SOUTH OF NATIONAL AVE
S-40-554	I-94	NB	1/4 MILE SOUTH OF AIRPORT EXIT
S-40-555	I-94	NB	COLLECTOR DIST. STH 119 AIRPORT EXIT
S-40-581	I-894	EB	W-S RAMP
S-40-585	I-94	SB	MITCHELL INTERCHANGE W-S RAMP
S-40-834	I-894	WB	AT LOOMIS RD
S-40-844	STH 145	SB	AT GRANTOSA DRIVE
S-40-903	STH 175 NB	NB	ON RAMP TO USH 45 RAMP METERS
S-40-907	STH 241 SB	SB	AT RAWSON
S-40-918	STH 181	NB	AT STH 100 ENTRANCE RAMP
S-40-926	COUNTY PP E	EB	AT I- 43 APPROACHING CTH W

SIGN BRIDGE LOCATIONS - WAUKESHA COUNTY

STRUCTURE NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION
S-67-215	COUNTY T	NB	OVERPASS I-94
S-67-216	COUNTY T	SB	OVERPASS I-94
S-67-230	STH 164	NB	SOUTH OF CTH ES
S-67-234	STH 59 WB	WB	AT BARKER
S-67-236	COUNTY Y	NB	AT GREENFIELD
S-67-238	STH 145 SB	SB	1/8 MILE NORTH OF US 41/4
S-67-243	STH 145 SB	SB	NORTH INTERCHANGE
S-67-900	I-94	EB	EXIT RAMP TO CTH P
S-67-901	I-94	WB	EXIT RAMP TO CTH P
S-67-902	COUNTY P	SB	AT ENTRANCE RAMPS TO 1- 94
S-67-903	COUNTY P	NB	AT ENTRANCE RAMPS TO 1- 94
S-67-904	COUNTY P	SB	SB AT ENTRANCE RAMPS T I- 94
S-67-905	PABST FARD RD	EB	APPROACHING CTH P
S-67-909	STH 67	SB	AT CTH B
S-67-929	STH 83	NB	AT CTH I
S-67-930	STH 83	SB	AT CTH I
S-67-931	STH 83	SB	AT CTH X

PROJECT NO: 1000-20-68

HWY: VARIOUS

COUNTY: SOUTHEAST REGION WIDE

SIGN REPAIR LOCATIONS

Ε

FILE NAME: 02_02_location.dgn

4:07:26 PM

PLOT BY: Veronica Chavezv8I

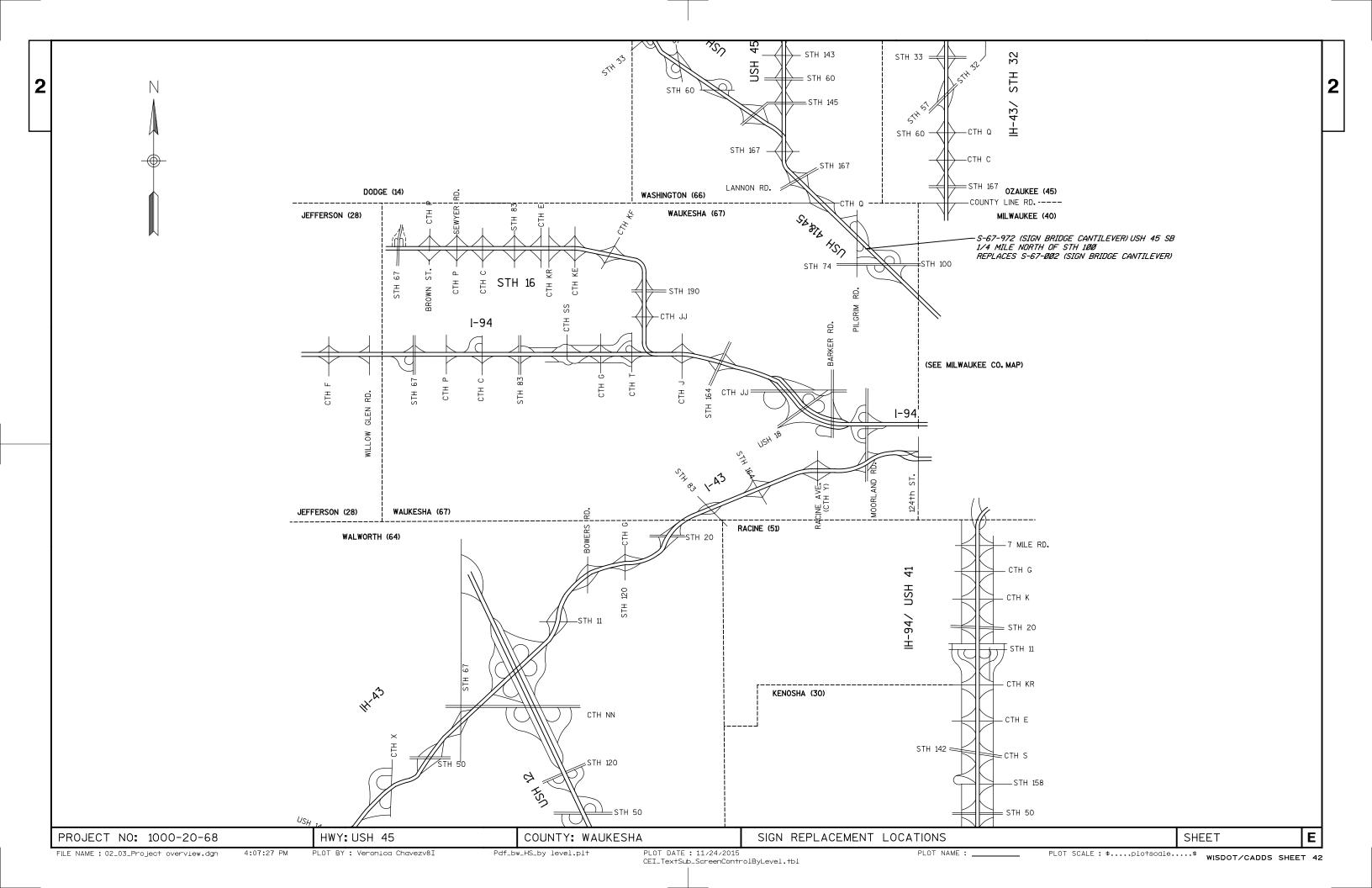
Pdf_bw_HS_by level.pl+

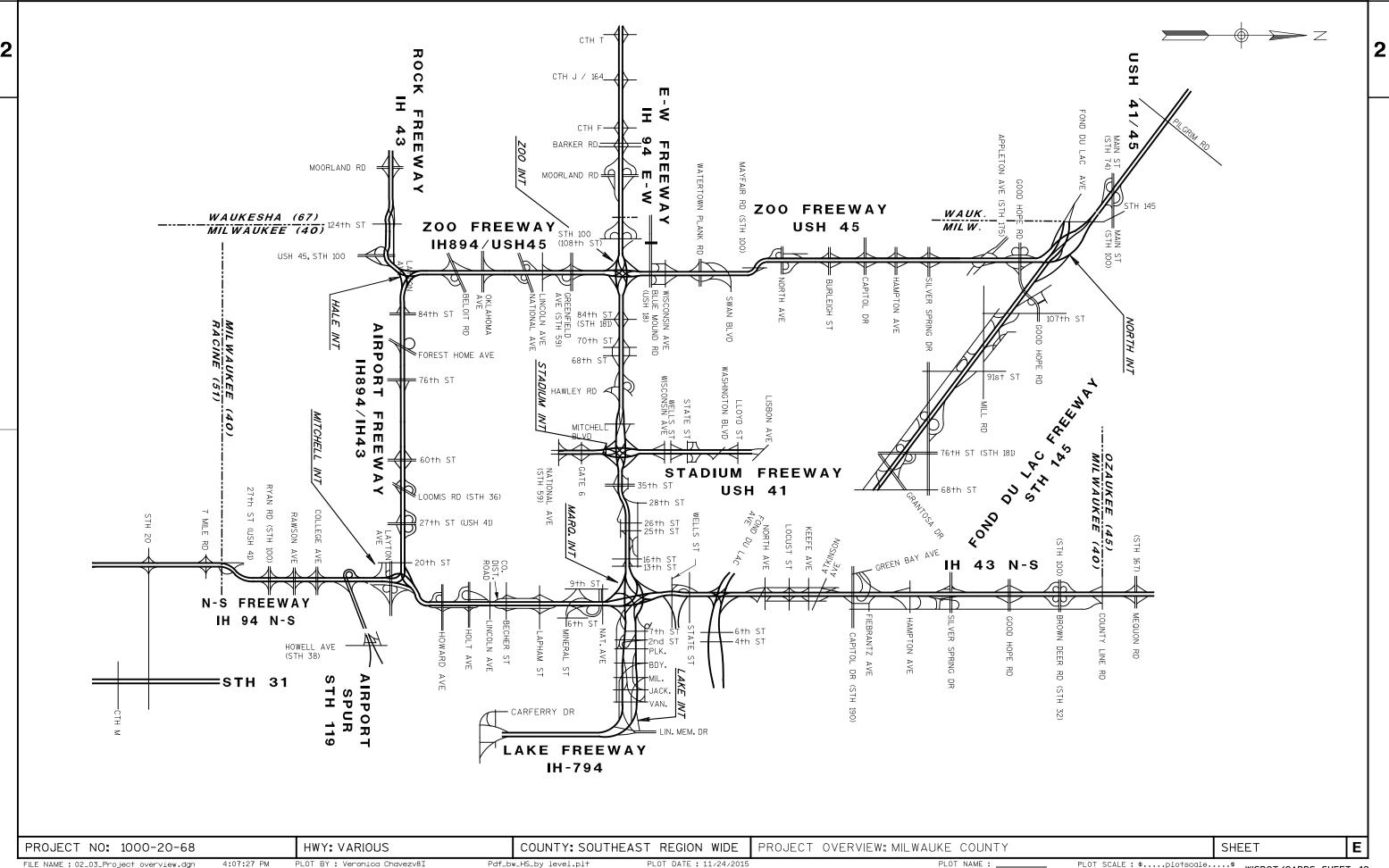
PLOT DATE: 11/24/2015

CEI_TextSub_ScreenControlByLevel.tbl

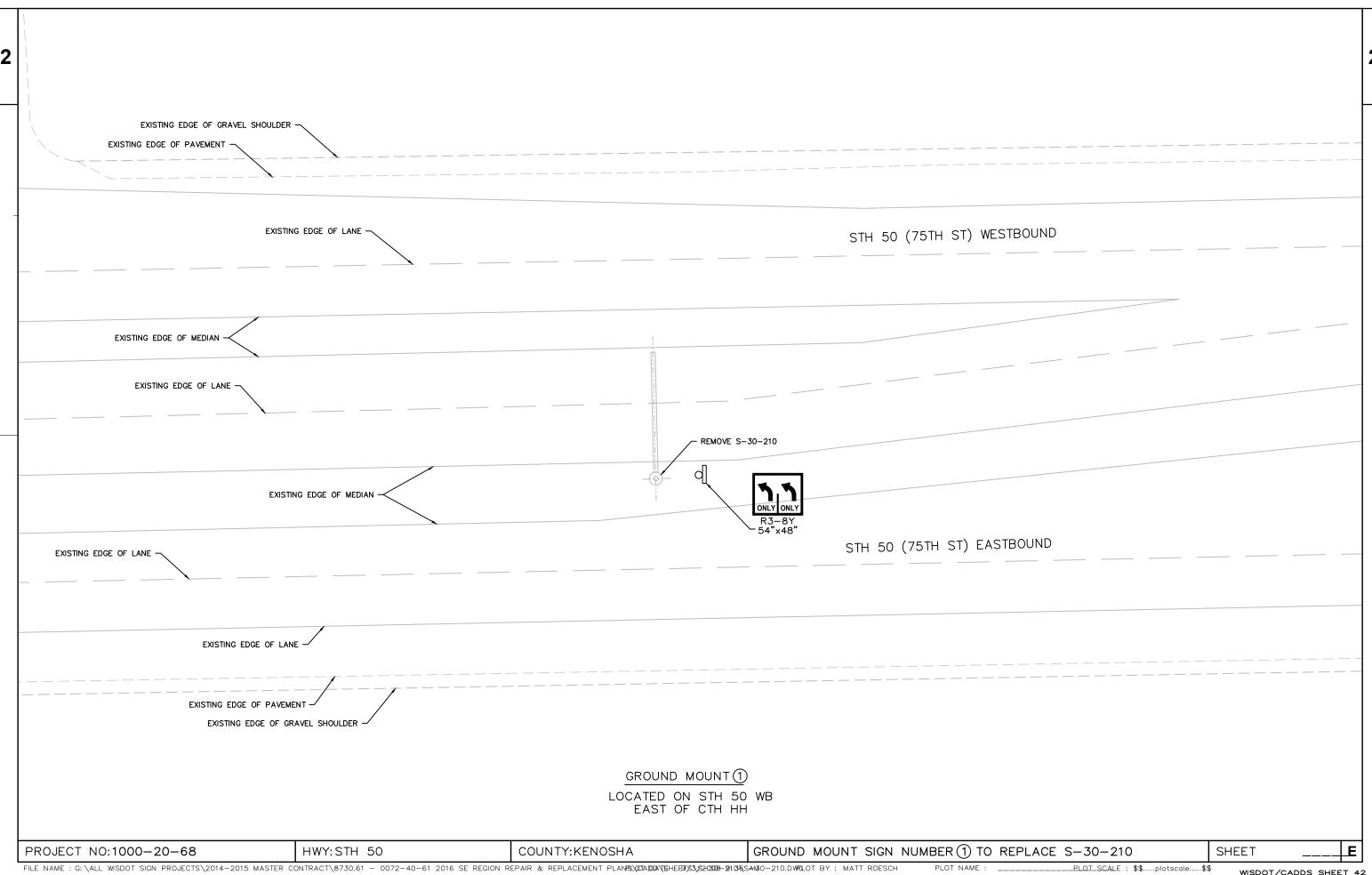
PLOT NAME:

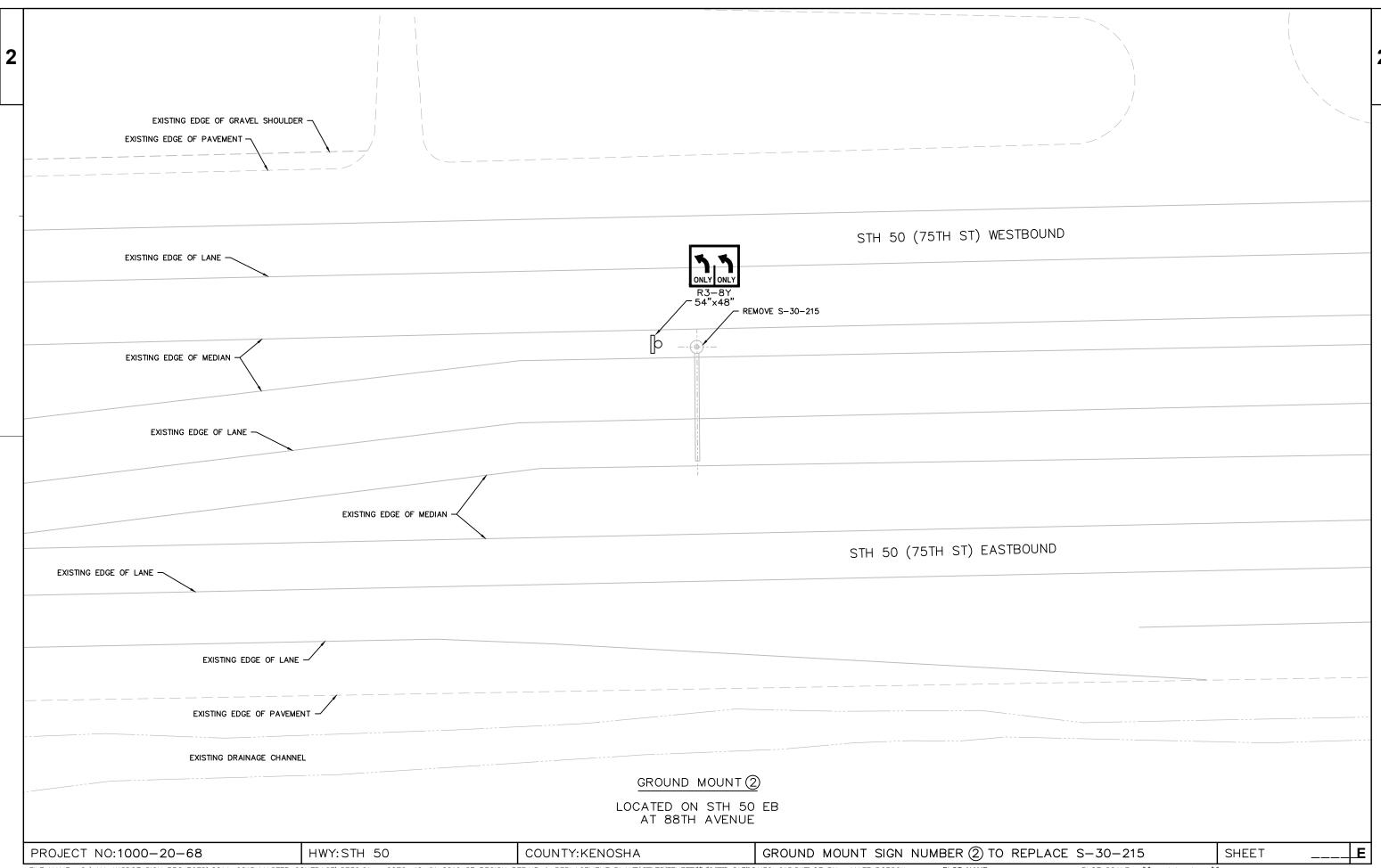
PLOT SCALE: \$.....plotscale.....\$ WISDOT/CADDS SHEET 42



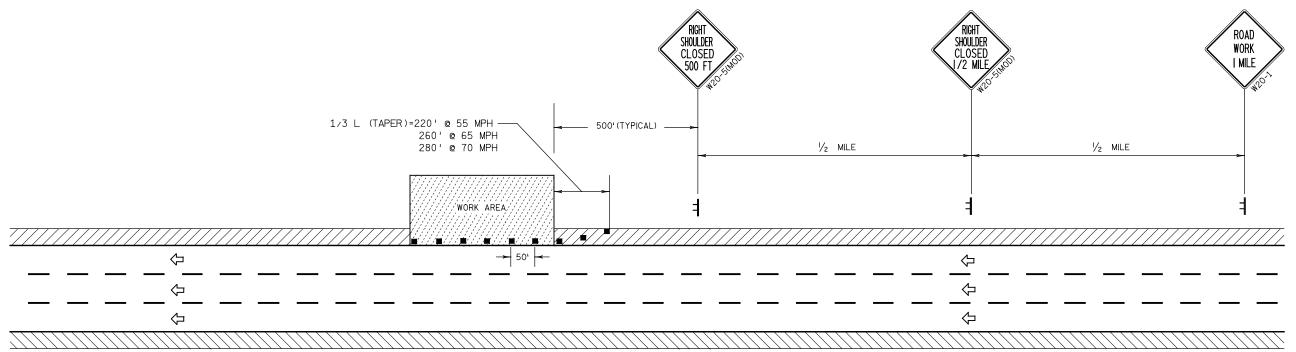


PLOT SCALE: \$.....plotscale.....\$ wisdot/cadds sheet 42 4:07:27 PM PLOT BY: Veronica Chavezv8I PLOT NAME : Pdf_bw_HS_by level.plt PLOT DATE: 11/24/2015 CEI_TextSub_ScreenControlByLevel.tbl





SHOULDER CLOSURE



MEDIAN

LEGEND

SIGN ON TEMPORARY SUPPORT

■ TRAFFIC CONTROL DEVICE

ARROW BOARD

SPECIAL APPLICATION NOTES

THE TAPER SHOULD EXTEND ACROSS THE SHOULDER, UNLESS DOING SO WOULD GREATLY CONFLICT WITH THE WORK OPERATION.

ALL LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL ARROWBOARDS AND DEVICES REMOVED BEYOND THE SHOULDER WHEN THE WORK IS NOT IN PROGRESS AND THE LANE IS RESTORED TO A SAFE OPERATING CONDITION.

GENERAL NOTES FOR TRAFFIC CONTROL

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM TRAVEL LANE AND OFF OF SHOULDER WHEN WORK IS NOT IN PROGRESS.

DURING HOURS OF DARKNESS, TYPE "C" (STEADY BURN) LIGHTS SHALL BE PROVIDED ON ALL CHANNELIZING DEVICES IN TAPERS. BARRICADES SHIELDING AN ISOLATED HAZARD, SHALL BE EQUIPPED WITH TYPE "A" (LOW-INTENSITY FLASHING) LIGHTS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS

IF LANE CLOSURE IS MORE THAN 1 MILE, PLACE A TYPE III BARRICADE APPROXIMATELY EVERY 1000'ACROSS THE CLOSED LANE TO HELP ENFORCE

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROWBOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500'IN FRONT OF DRUMS.

PROJECT NO: 1000-20-68

HWY: VARIOUS

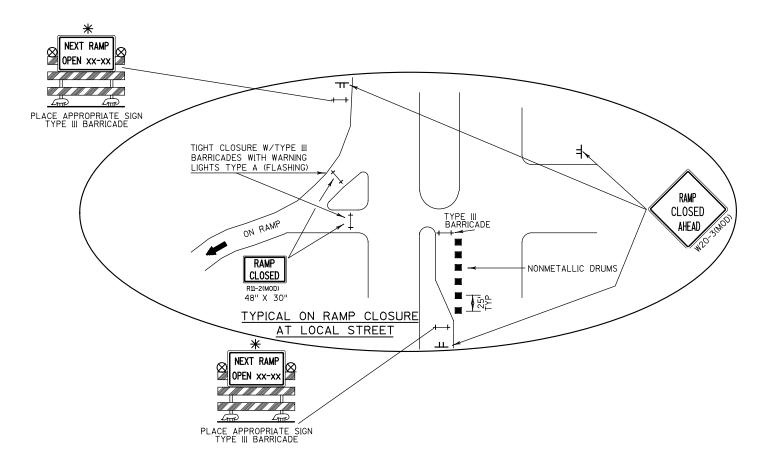
Pdf_bw_HS_by level.plt

COUNTY: SOUTHEAST REGION WIDE

TRAFFIC CONTROL DEVICES FOR HIGHWAY MAINTENANCE PLOT NAME :

PLOT DATE: 11/24/2015 CEI_TextSub_ScreenControlByLevel.tbl 2

2



TRAFFIC CONTROL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

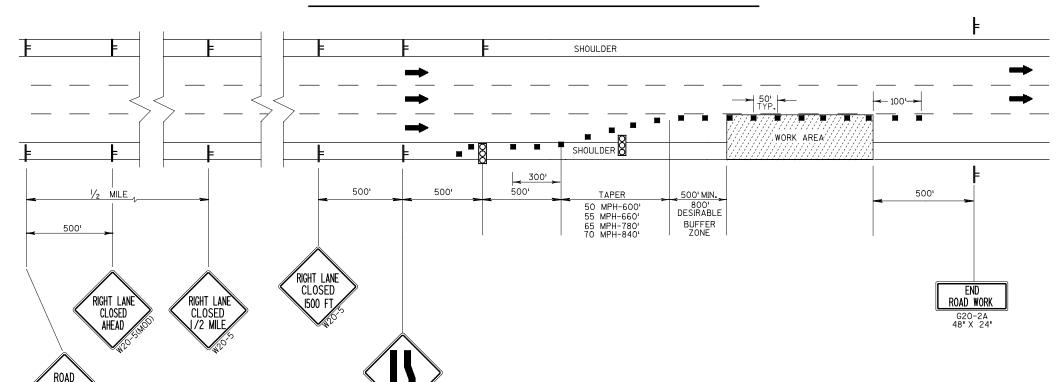
"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

LEGEND

- TRAFFIC CONTROL DEVICE
- F SIGN ON TEMPORARY SUPPORT FOR SHORT TERM 3 DAYS OR LESS
- TYPE III BARRICADE
- ₩ARNING LIGHT, TYPE A (FLASHING)

PROJECT NO: 1000-20-68 HWY: VARIOUS COUNTY: SOUTHEAST REGION WIDE TRAFFIC CONTROL FOR ON-RAMP CLOSURE (SHORT TERM) SHEET E

TYPICAL ONE-LANE CLOSURE (SHORT TERM-3 DAYS OR LESS)



LEGEND

WORK

TRAFFIC CONTROL DEVICE

SIGN ON FIXED SUPPORT

SIGN ON TEMPORARY SUPPORT

ARROW BOARD

TRAFFIC CONTROL NOTES

RIGHT LANE CLOSURE SHOWN, LEFT LANE CLOSURE SIMILAR.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

FOR NIGHTIME OPERATION ALL DRUMS IN TAPERS SHALL HAVE A WARNING LIGHT, TYPE C (STEADY BURN).

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

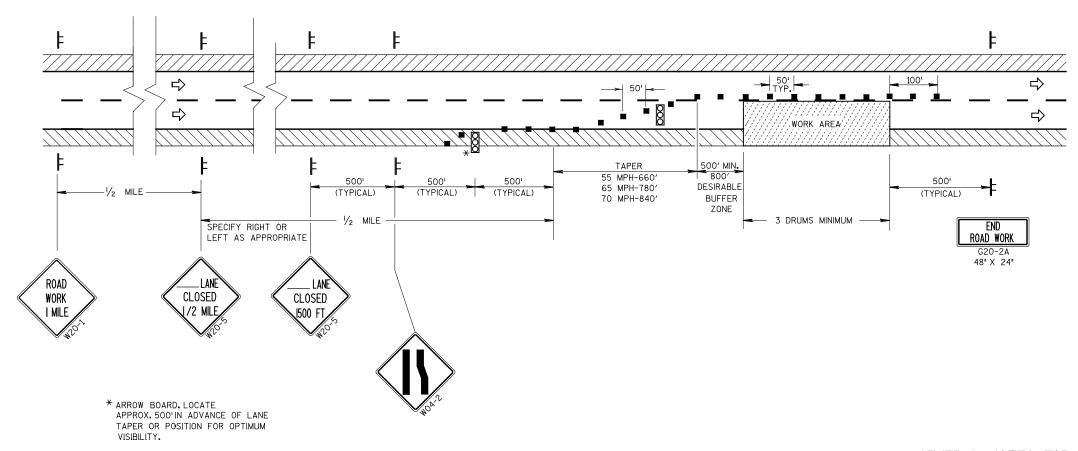
ALL SHORT TERM LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL ARROWBOARDS AND DEVICES REMOVED BEYOND THE SHOULDER WHEN THE WORK IS NOT IN PROGRESS AND THE LANE IS RESTORED TO A SAFE OPERATING CONDITION.

IF LANE CLOSURE IS MORE THAN 1 MILE, PLACE A TYPE III BARRICADE APPROXIMATELY EVERY 1000 ACROSS THE CLOSED LANE TO HELP ENFORCE THE DRUM LINE.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROWBOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500' IN FRONT OF DRUMS.

PROJECT NO: 1000-20-68 HWY: VARIOUS COUNTY: SOUTHEAST REGION WIDE TRAFFIC CONTROL FOR CLOSING ONE LANE OF FREEWAY (SHORT TERM) SHEET E

TYPICAL ONE-LANE CLOSURE (SHORT TERM - 3 DAYS OR LESS)



LEGEND

SIGN ON TEMPORARY SUPPORT

TRAFFIC CONTROL DEVICE

ARROW BOARD

SPECIAL APPLICATION NOTES

THE TAPER SHOULD EXTEND ACROSS THE SHOULDER, UNLESS DOING SO WOULD GREATLY CONFLICT WITH THE WORK OPERATION.

ALL LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL ARROWBOARDS AND DEVICES REMOVED BEYOND THE SHOULDER WHEN THE WORK IS NOT IN PROGRESS AND THE LANE IS RESTORED TO A SAFE OPERATING CONDITION.

PLOT DATE: 11/24/2015

CEI_TextSub_ScreenControlByLevel.tbl

GENERAL NOTES FOR TRAFFIC CONTROL

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM TRAVEL LANE AND OFF OF SHOULDER WHEN WORK IS NOT IN PROGRESS.

DURING HOURS OF DARKNESS, TYPE "C" (STEADY BURN) LIGHTS SHALL BE PROVIDED ON ALL CHANNELIZING DEVICES IN TAPERS. BARRICADES SHIELDING AN ISOLATED HAZARD, SHALL BE EQUIPPED WITH TYPE "A" (LOW-INTENSITY FLASHING) LIGHTS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

IF LANE CLOSURE IS MORE THAN 1 MILE, PLACE A TYPE III BARRICADE APPROXIMATELY EVERY 1000'ACROSS THE CLOSED LANE TO HELP ENFORCE

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROWBOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500'IN FRONT OF DRUMS.

RIGHT LANE CLOSURE SHOWN, LEFT LANE CLOSURE SIMULAR.

PROJECT NO: 1000-20-68

HWY: VARIOUS

COUNTY: SOUTHEAST REGION WIDE

TRAFFIC CONTROL DEVICES FOR MAINTENANCE LANE CLOSURE (SHORT TERM)

PLOT NAME :

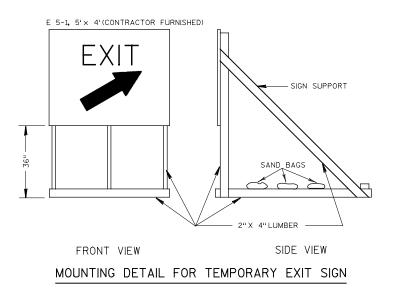
Ε

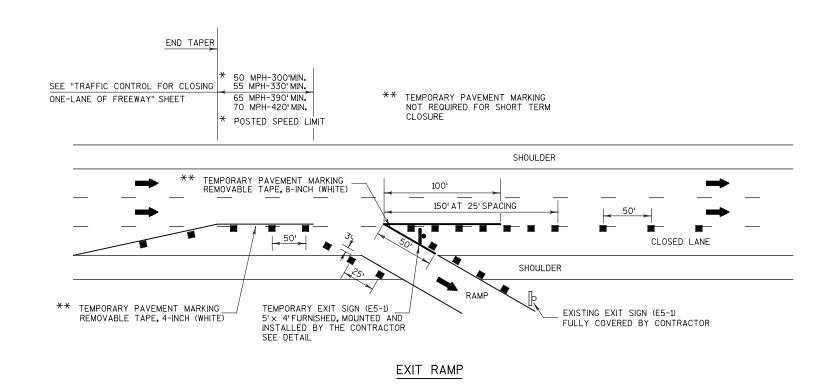
GENERAL NOTES

- 1. ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED.
- 2. FOR NIGHTIME OPERATION ALL DRUMS SHALL HAVE A WARNING LIGHT, TYPE C (STEADY BURN).
- 3. FOR SHORT TERM (3 DAYS OR LESS) FREEWAY LANE CLOSURES TEMPORARY SIGN SUPPORTS MAY BE USED. REMOVE OR COVER SIGNS WHEN NOT IN USE.

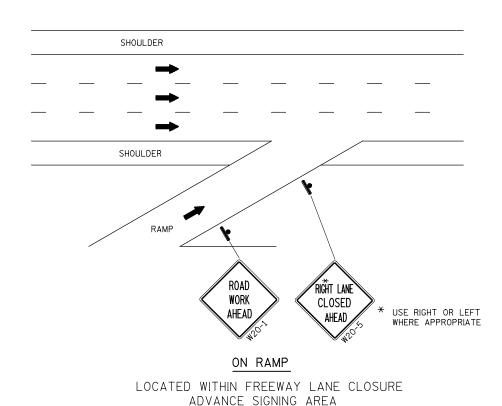
LEGEND

- - TRAFFIC CONTROL DEVICE
- → SIGN ON TEMPORARY SUPPORT



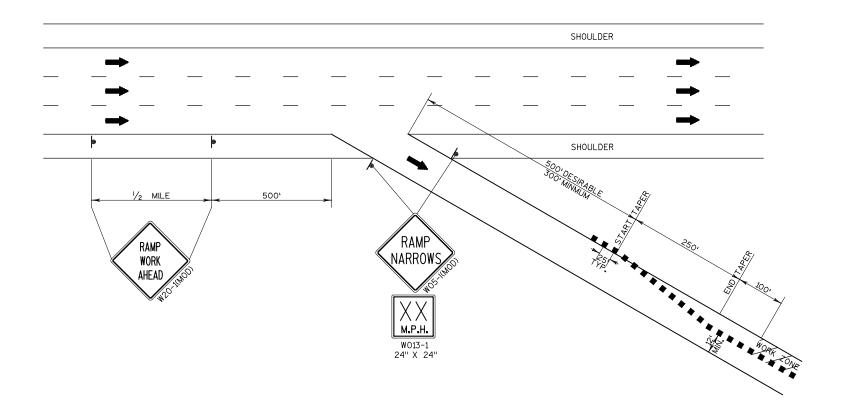


LOCATED WITHIN CLOSED FREEWAY LANE AREA



PROJECT NO: 1000-20-68 COUNTY: SOUTHEAST REGION WIDE TRAFFIC CONTROL FOR SERVICE RAMPS Ε HWY: VARIOUS PLOT SCALE: \$.....plotscale.....\$ WISDOT/CADDS SHEET 42 Pdf_bw_HS_by level.plt PLOT DATE: 11/24/2015

TYPICAL PARTIAL EXIT RAMP CLOSURE (SHORT TERM-3 DAYS OR LESS)



LEGEND

- TRAFFIC CONTROL DEVICE
- SIGN ON TEMPORARY SUPPORT
- WARNING LIGHT, TYPE A (FLASHING)

TRAFFIC CONTROL NOTES

LEFT SIDE CLOSURE SHOWN, RIGHT SIDE CLOSURE SIMILAR.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

FOR NIGHTIME OPERATION ALL DRUMS IN TAPERS SHALL HAVE A WARNING LIGHT, TYPE "C" (STEADY BURN).

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

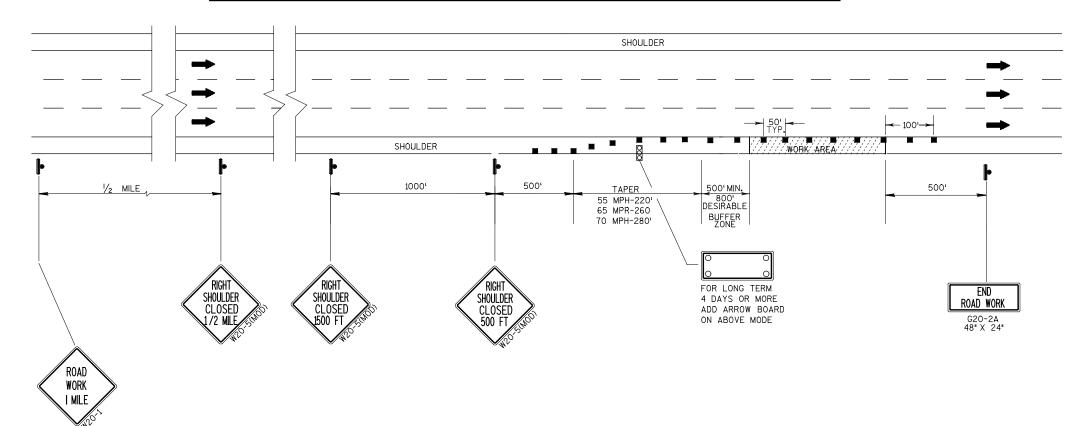
ALL SHORT TERM LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL ARROWBOARDS AND DEVICES REMOVED BEYOND THE SHOULDER WHEN THE WORK IS NOT IN PROGRESS AND THE ROADWAY IS RESTORED TO A SAFE OPERATING CONDITION.

SPEED RECOMMENDED BY WO5-1(MOD) SIGN SHALL BE 5 M.P.H. BELOW ADVISORY EXIT RAMP SPEED OR AS DIRECTED BY THE ENGINEER.

SIGNING ON FREEWAY IN ADVANCE OF EXIT RAMP GORE SHALL BE LOCATED ON THE SAME SIDE OF THE FREEWAY AS THE EXIT RAMP.

CEI_TextSub_ScreenControlByLevel.tbl

TYPICAL SHOULDER CLOSURE (SHORT TERM-3 DAYS OR LESS OR 4 DAYS OR MORE)



LEGEND

- TRAFFIC CONTROL DEVICE
- | SIGN ON TEMPORARY SUPPORT (FOR 3 DAYS OR LESS SHORT TERM)
- SIGN ON FIXED SUPPORT FOR 4 DAYS OR MORE (LONG TERM)
- ARROW BOARD

TRAFFIC CONTROL NOTES

RIGHT LANE CLOSURE SHOWN, LEFT LANE CLOSURE SIMILAR.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

FOR NIGHTIME OPERATION ALL DRUMS IN TAPERS SHALL HAVE A WARNING LIGHT, TYPE C (STEADY BURN).

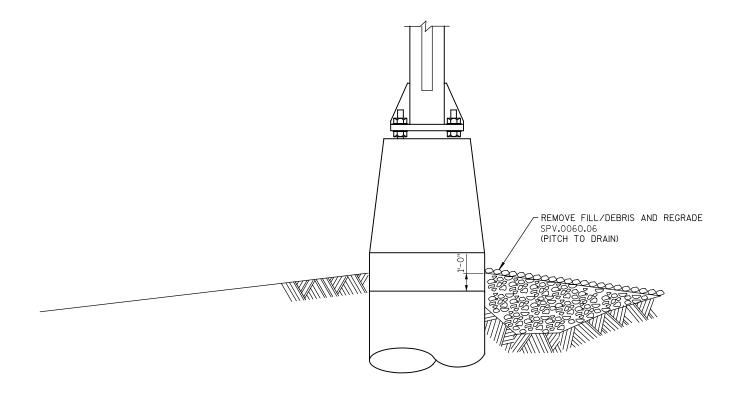
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

ALL SHORT TERM LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL ARROWBOARDS AND DEVICES REMOVED BEYOND THE SHOULDER WHEN THE WORK IS NOT IN PROGRESS AND THE LANE IS RESTORED TO A SAFE OPERATING CONDITION.

PROJECT NO: 1000-20-68 HWY: VARIOUS COUNTY: SOUTHEAST REGION WIDE TRAFFIC CONTROL FOR FREEWAY SHOULDER CLOSURE SHEET **E**



PROJECT NO: 1000-20-68 HWY: VARIOUS 4:07:30 PM

PLOT BY: Veronica Chavezv8I

COUNTY: SOUTHEAST REGION WIDE | CONSTRUCTION DETAILS

Ε

FILL EROSION

LINE	7JAN16	E S T	IMATE	OF QUANT	T I T I E S 1000-20-68	
NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	QUANTI TY	
0010	619. 1000	Mobilization	EACH	1.000	1. 000	
0020	634. 0616	Posts Wood 4x6-Inch X 16-FT	EACH	4. 000	4. 000	
0030	636. 0100	Sign Supports Concrete Masonry	CY	8.000	8. 000	
0040 0050	636. 1000 637. 1220	Sign Supports Steel Reinforcement HS Signs Type I Reflective SH	LB SF	980. 000 200. 500	980. 000 200. 500	
0030	037. 1220	signs type i kerrective sn	эг	200. 500	200. 500	
0060	637. 2210	Signs Type II Reflective H	SF	36. 000	36. 000	
0070	637. 2215	Signs Type II Reflective H Folding	SF	18.000	18. 000	
0800	641. 1200	Sign Bridge Cantilevered (structure) 01.	LS	1. 000	1. 000	
0000	442 0100	S-67-972 Traffic Control (project) 01. 1000-20-68	EACH	1 000	1 000	
0090 0100	643. 0100 SPV. 0060	Special O1. SIGN STRUCTURE ANCHOR ROD	EACH EACH	1. 000 11. 000	1. 000 11. 000	
0100	3F V. 0000	REPAIR	LACII	11.000	11.000	
0110	SPV. 0060	Special O2. REMOVE GROUT PAD	EACH	4. 000	4. 000	
0120	SPV. 0060	Special 03. REPLACE RODENT SCREEN	EACH	9. 000	9. 000	
0130 0140	SPV. 0060 SPV. 0060	Special O4. TENSION ANCHOR RODS Special O5. REPAIR FOUNDATION	EACH EACH	71. 000 1. 000	71. 000 1. 000	
0140	SPV. 0060	Special O6. REMOVE FILL/DEBRIS AND	EACH	5. 000	5. 000	
0130	31 V. 0000	REGRADE	LACIT	3.000	3.000	
0160	SPV. 0060	Special O7. SPLICE CONNECTION BOLT	EACH	93.000	93. 000	
0170	CDV COVC	REPLACEMENT	EACH	7 000	7 000	
0170	SPV. 0060	Special 08. TIGHTEN/REPLACE POST TO MAST ARM CONNECTION	EACH	7. 000	7. 000	
0180	SPV. 0060	Special 09. SECURE/REPLACE CHORD CAP	EACH	1. 000	1. 000	
0190	SPV. 0060	Special 10. REINSTALL TRUSS	EACH	1. 000	1. 000	
0200	SPV. 0060	Special 11. TENSION TRUSS CONNECTION	EACH	3. 000	3. 000	
		BOLT				
0010	CDV 00/0	Constal 40 TENCION CARRIE ROCT TO		2 000	2.000	
0210	SPV. 0060	Special 12. TENSION SADDLE POST TO TRUSS/CHORD CONNECTION BOLT	EACH	2. 000	2. 000	
0220	SPV. 0060	Special 13. CLEAN NESTING DEBRIS	EACH	1. 000	1. 000	
0230	SPV. 0060	Special 14. INSTALL U-BOLT	EACH	3. 000	3. 000	
0240	SPV. 0060	Special 15. INSTALL VERTICAL SIGN	EACH	2. 000	2. 000	
		SUPPORT BRACKET/SUPPORT BOLTS				
0250	SPV. 0060	Special 16. REPLACE SIGN ID BRIDGE	EACH	10. 000	10. 000	
		PLAQUE				
0260	SPV. 0060	Special 17. REPLACE SIGN PANEL CONNECTOR	EACH	61. 000	61. 000	
0200	SPV. 0060	Special 18. REATTACH TYPE II SIGN	EACH	2. 000	2. 000	
0280	SPV. 0060	Special 19. INSTALL OVERHEAD SIGN	EACH	2. 000	2. 000	
		MOUNTING SYSTEM				
0290	SPV. 0060	Special 20. REPLACE SIGN CONNECTION	EACH	1. 000	1. 000	
0200	SDV 0040	ASSEMBLEY	EACH	07 000	07 000	
0300	SPV. 0060	Special 21. SLOTTED VERTICAL SIGN SUPPORT REPAIR	EACH	87. 000	87. 000	
		SOLIVILI KLI MIK				
0310	SPV. 0060	Special 22. INSTALL VERTICAL SIGN	EACH	7. 000	7. 000	
		SUPPORT				
0320	SPV. 0060	Special 23. SECURE HANDHOLE COVER	EACH	1. 000	1. 000	
0330	SPV. 0060	Special 24. SECURE LUMINAIRE COVER	EACH	4.000	4. 000	
0340	SPV. 0060	Special 25. INSTALL JUNCTION BOX COVER	EACH	1.000	1. 000	
0350	SPV. 0060	Special 26. TIGHTEN SIGNAL MOUNT SOCKET CONNECTION	EACH	2. 000	2. 000	
		CONNECTION				
0360	SPV. 0060	Special 27. REPLACE CONDUIT PLUG	EACH	1. 000	1. 000	
0370	SPV. 0060	Special 28. REMOVE ASIM TRAFFIC COUNTER	EACH	26.000	26. 000	
	001: -	UNI TS	=			
0380	SPV. 0060	Special 29. REMOVE LUMINAIRE	EACH	2.000	2. 000	
0390	SPV. 0060 SPV. 0060	Special 30. FILL EROSION Special 31. REPLACED DISHED WASHER	EACH EACH	2. 000 4. 000	2. 000 4. 000	
0400			CHULT	4 (1(1)	4 (////	

DATE 27	JAN16	E S	TIMAT	E OF QUANT	I T I E S 1000-20-68
NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	QUANTI TY
0410	SPV. 0060	Special 32. REPLACE MISSING HANDHOLE COVER	EACH	2. 000	2. 000
0420	SPV. 0060	Special 33. REPLACE SAFTEY CHAIN	EACH	2.000	2.000
0430	SPV. 0060	Special 34. REPLACE TRUSS MEMBER	EACH	1. 000	1. 000
0440	SPV. 0060	Special 35. SECURE/REPLACE VERTICAL POST CAP	EACH	2. 000	2. 000
0450	SPV. 0060	Special 36. TENSION CANTILEVER POST TO TRUSS CONNECTION BOLT	EACH	2.000	2. 000
0460	SPV. 0060	Special 37. TIGHTEN LOOSE U-BOLT	EACH	4. 000	4. 000
0470	SPV. 0060	Special 38. TIGHTEN/REPLACE HANDHOLE COVER BOLT	EACH	2. 000	2. 000
0480	SPV. 0060	Special 39. ALIGN LOCKING PIN	EACH	2.000	2.000
0490	SPV. 0060	Special 40. TENSION TRUSS GUSSET CONNECTION BOLT	EACH	2.000	2. 000
0500	SPV. 0090	Special 01. INSTALL/REPLACE CATWALK HANDRAIL	LF	4.000	4. 000
0510	SPV. 0105	Special 01. REMOVING OVERHEAD SIGN SUPPORT S-30-210	LS	1. 000	1. 000
0520	SPV. 0105	Special O2. REMOVING OVERHEAD SIGN SUPPORT S-30-215	LS	1. 000	1. 000
0530	SPV. 0105	Special O3. PARTIAL REMOVAL OF SIGN STRUCTURE FOUNDATION	LS	1. 000	1. 000
0540	SPV. 0165	Special O1. REPLACE TYPE-II SIGN	SF	20. 000	20. 000

3

STRUCTURE NUMBER	ELEVATION VIEW	HIGHWAY	REMOVE FILL/DEBRIS AND REGRADE
	SHEET NUMBER		SPV.0060.06
			EACH
S-30-610	20 OF 104	COUNTY EZ	1
S-40-436	50 OF 104	I-894	1
S-40-844	67 OF 104	STH 145	1
S-40-907	69 OF 104	STH 241 SB	1
S-67-234	89 OF 104	STH 59 WB	1
UNDISTRIBUTED			
TOTAL			5

^{*} THE INFORMATION PROVIDED IN THIS TABLE IS FOR INFORMATION PURPOSES ONLY AND NOT TO BE COUNTED AS ADDITIONAL QUANTITIES TO THOSE LISTED ON THE ELEVATION VIEW SHEETS.

SIGN REPAIR MISCELLANEOUS QUANTITIES Ε PROJECT NO: 1000-20-68 HWY: VARIOUS COUNTY: SE REGION WIDE SHEET

SIGN BRIDGE REMOVALS LOCATION	SPV.0105.01 REMOVING OVERHEAD SIGN SUPPORT S-30-210 LS	SPV.0105.02 REMOVING OVERHEAD SIGN SUPPORT S-30-215 LS				SIGN BRIDGES LOCATION	641.1200 SIGN BRIDGE CANTILEVER S-67-972 LS	637.1220 SIGNS TYPE I REFLECTIVE SH SF	
STH 50 EAST OF CTH HH	1					USH 45 1/4 MILE NORTH OF STH 100	1	200.5	
STH 50 AT 88TH AVENUE		1				TOTAL	1	200.5	
TOTAL	1	1		CATEGORY CODE 0020					CATEGORY CODE 002
SIGNS				637.2215 637	2210 634 0	616			
	STH 50 EAST OF CTH HH STH 50 AT 88TH AVENUE TOTAL	SPV.0105.01 REMOVING OVERHEAD SIGN SUPPORT S-30-210 LS STH 50 EAST OF CTH HH 1 STH 50 AT 88TH AVENUE TOTAL 1	SPV.0105.01 SPV.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT SIGN SUPPORT S-30-210 S-30-215 LS STH 50 EAST OF CTH HH 1 TOTAL 1 1 SPV.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT S-30-210 S-30-215 LS 1 1 1 1	SPV.0105.01 SPV.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT S-30-210 S-30-215 LS LS STH 50 EAST OF CTH HH 1 TOTAL 1 SPV.0105.02 REMOVING OVERHEAD SIGN SUPPORT S-30-215 LS 1 1 TOTAL 1 1	SPV.0105.01 SPV.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT SIGN SUPPORT S-30-210 S-30-215 LS EAST OF CTH HH 1 TOTAL 1 1 CATEGORY CODE 0020	SPV.0105.01 SPV.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT SIGN SUPPORT S-30-210 S-30-215 LS LS STH 50 EAST OF CTH HH 1 TOTAL 1 1 1 CATEGORY CODE 0020	SPV.0105.01 SPV.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT SIGN SUPPORT S-30-215 S-30-210 S-30-215 LS LOCATION EAST OF CTH HH 1 TOTAL 1 1 1 CATEGORY CODE 0020	SPV.0105.01 SPV.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT SIGN SUPPORT SIGN SUPPORT SIGN SUPPORT S-30-210 S-30-215 LS Location S-30-215 LS	SPY.0105.01 SPY.0105.02 REMOVING OVERHEAD REMOVING OVERHEAD SIGN SUPPORT SIGN SUPPORT

SIZE SIGN SIGN SIZE SIGN FOLDING SIGN NO. FT X FT MESSAGE CODE IN X IN MESSAGE SF SF EACH (ARROW) GROUND MOUNT TO REPLACE S-30-210. R3-8Y 4.5 X 4 18 18 2 ONLY REFER TO S-30-210 REMOVAL PLAN GROUND MOUNT TO REPLACE S-30-215. REFER TO S-30-215 REMOVAL PLAN (ARROW) R3-8Y 4.5 X 4 18 18 36 TOTAL

CATEGORY CODE 0010

3

Ε PROJECT NO: 1000-20-67 HWY: VARIOUS COUNTY: SE REGION WIDE SIGN REPLACEMENT MISCELLANEOUS QUANTITIES SHEET PLOT NAME : ____

FILE NAME: 03_quantity2.dgn PLOT DATE: 11/24/2015

4:0**7:**30 PM PLOT BY: Veronica Chavezv8I Pdf_bw_HS_by level.plt

PLOT SCALE: \$\$.....plo†scale.....\$\$ wisdot/cadds SHEET 43

STRUCTURE NUMBER	ELEVATION VIEW SHEET NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION	SIGN STRUCTURE ANCHOR ROD REPAIR	REMOVE GROUT	REPLACE RODENT SCREEN	TENSION ANCHOR RODS	REPAIR FOUNDATION	REMOVE FILL/DEBRIS ANI REGRADE	SPLICE D CONNECTION BOLT REPLACEMENT	TIGHTEN/REPLACE POST TO MAST ARM CONNECTION	SECURE/REPLACE CHORD CAP	REINSTALL TRUSS	TENSION TRUSS CONNECTION BOLT	TENSION SADDLE POST TO TRUSS/CHORD CONNECTION BOLT	CLEAN NESTING DEBRIS	INSTALL U-BOLT	INSTALL VERTICAL SIGN SUPPORT BRACKET/SUPPORT BOLTS	REPLACE SIGN T BRIDGE ID PLAQUE	REPLACE SIGN PANEL CONNECTOR	REATTACH TYPE-I SIGN	IREPLACE TYPE- SIGN	INSTALL OVERHEAD SIGN MOUNTING SYSTEM	REPLACE SIGN CONNECTION ASSEMBLY	SLOTTED VERTICAL SIGN SUPPORT REPAIR	INSTALL VERITCAL SIGN SUPPORT	L SECURE HANDHOLE COVER
	SHEET NUMBER				SPV.0060.01 EACH	SPV.0060.02 EACH	SPV.0060.03 EACH	SPV.0060.04 EACH	SPV.0060.05 EACH	SPV.0060.06 EACH	SPV.0060.07 EACH	SPV.0060.08 EACH	SPV.0060.09 EACH	SPV.0060.10 EACH	SPV.0060.11 EACH	SPV.0060.12 EACH	SPV.0060.13 EACH	SPV.0060.14 EACH	SPV.0060.15 EACH	SPV.0060.16 EACH	SPV.0060.17 EACH	SPV.0060.18 EACH	SPV.0165.01 SF	SPV.0060.19 EACH	SPV.0060.20 EACH	SPV.0060.21 EACH	SPV.0060.22 EACH	SPV.0060.23 EACH
6 70 005	0.05.404	1.04	ND	AT EVIT TO WEIGH CTATION	LHOII	LACIT	LAOI		EAGI	EAGI	LAGII	LAGII	LAGII	LAGII	LAGII	EROII	LAGII	LAGII	LAGIT	LACIT	LAGIT	LAOII	-	LAGII	LAGIT	LACII	EROII	EROII
S-30-025 S-30-035	9 0F 104 10 0F 104	I-94 I-94	NB SB	AT EXIT TO WEIGH STATION AT EXIT TO RUSSEL RD				1			2										2							
S-30-232	11 OF 104	STH 165	EB	AT CTH ML				1																		4		+
S-30-233	12 OF 104	STH 165 WB	WB	AT CTH ML				1																		4		+
S-30-234	13 OF 104	STH 165	WB	AT CTH EZ	2			2																		5		+
S-30-235	14 OF 104	STH 165 WB	WB	AT CTH EZ	2			2																		4		+
S-30-236	15 OF 104	STH 165	WB	APPROACHING 47TH AVE AFTER ML	1			1					1															+
S-30-250	16 OF 104	STH 31	SB	AFTER ML AT STH 50				1																				+
S-30-255	17 OF 104	WEIGH STATION ENTRANCE RD	NB	WEIGH STATION ENTRANCE RD			1	-												1								+
S-30-608	18 OF 104	COUNTY ML	NB	AT STH 165			•	2			1															4		
S-30-609	19 OF 104	COUNTY ML	SB	AT STH 165																						4		+
S-30-610	20 OF 104	COUNTY EZ	SB	AT STH 165	1			2		1																4		+
S-30-611	21 OF 104	COUNTY EZ	SB	AT STH 165	2			1		-																4		+
S-30-612	22 OF 104	WEST FRONTAGE RD	SB	AT 71ST ST EAST OF I- 94 FRONTAGE RD				<u> </u>												1						· ·		
S-40-009	23 OF 104	I-894	WB	AT 60TH STREET EXIT																	16							+
S-40-018	24 OF 104	L-894	SB	I-894 @ THE HALE INTERCHANGE																	-						7	
S-40-049	25 OF 104	STH 100	NB	SOUTH OF USH 45 RAMP				2																				+
S-40-120	26 OF 104	I-43	NB	HALE INT SPLIT WITH USH 45		4		2																				
S-40-126	27 OF 104	I-894	SB	EXIT TO OKLAHOMA				1																				+
S-40-151	28 OF 104	1-94	WB	AT EXIT TO HAWLEY RD							8																	+
S-40-185	29 OF 104	STH 145 SB	EB	AT 14TH ST & WALNUT ST							4																	+
S-40-192		LINCOLN MEMORIAL DR	NB	AT I-794				2																				+
S-40-193		LINCOLN MEMORIAL DR	SB	AT MICHIGAN ST				2										1										+
S-40-204	32 OF 104	I-94	EB	AT 41ST ST				_										-				1						+
S-40-228	33 OF 104	1-43	SB	AT CAPITOL DR				1														•						+
S-40-229	34 OF 104	1-43	NB	AT C & NW RAILROAD				<u> </u>	1																			+
S-40-268	35 OF 104	HIGHWAY	EB	APPROACHING LAKE PKWY - STH 794								4																+
S-40-269	36 OF 104	HOWARD AVE	WB	STH 794 APPROACHING LAKE PKWY - STH 794							1	1																+
S-40-283	37 OF 104	STH 32 SB	SB	STH 794 COLUMBIA				1			•																	+
S-40-285	38 OF 104	STH 145 WB	WB	APPROACHING HILLAISW INTERCHANGE							1																	+
S-40-301	39 OF 104	STH 794 S	SB	NORTH OF E RUSSEL AVE			1													1								+
S-40-306	40 OF 104	GOOD HOPE RD	WB	GOOD HOPE			•				8									•								+
S-40-306	_	USH 41 SB-STH 175 SB	SB	AT USH 41				2			+ *							+										+
S-40-327	42 OF 104	USH 41	NB	AT MILL RD			1	2							1						40							+
S-40-328	43 OF 104	STH 145 NB	NB	AT PARK PLACE			1	2							<u> </u>			+			"							+
S-40-352	44 OF 104	USH 45	SB	AT HAMPTON OFF RAMP			-	<u> </u>										+										+
S-40-355	45 OF 104	USH 45	SB	AT NORTH AVE WEST																								
S-40-393	46 OF 104	I-43	SB	AT EXIT TO LAPHAM/MITCHELL ST																								+
S-40-423	47 OF 104	GOOD HOPE RD	EB	AT USH 41/45 ON RAMP, PRIOR TO RAMP			1																					+
S-40-425	48 OF 104	HIGHWAY	EB	BEFORE STH 100			-											2										+
S-40-426	49 OF 104	STH 175 SB	SB	BEFORE HWY 41							2							-										
S-40-436	50 OF 104	I-894	EB	BLANK OUT SIGNS AT TUNNEL - X & DOWN ARROW				1		1								+										+
S-40-437	51 OF 104	I-894	EB	BLANK OUT SIGNS IN ADV OF TUNNEL - X & DOWN ARROW				2																				+
S-40-438	52 OF 104	I-94		S-W CONNECTOR BLANK OUT SIGNS AT TUNNEL			1	<u> </u>												1								+
S-40-443	53 OF 104	I-94	WB	EAST OF 68TH ST			-																					+
S-40-502	54 OF 104	I-43	NB	SOUTH OF STH 145 EXIT																								+
S-40-503	55 OF 104	I-43		OFF RAMP TO MCKINLEY AVE																								+
S-40-512	56 OF 104	l-43	SB	1/4 MILE NORHT OF I-94																								+

REFER TO SHEET 3.3 FOR A CONTINUATION OF REPAIRS FOR THESE SIGN BRIDGES

MISCELLANEOUS QUANTITIES PROJECT NO: 1000-20-68 HWY: VARIOUS COUNTY: SE REGION WIDE SHEET 3.1 OF 3.4

3

	STRUCTURE NUMBER	ELEVATION VIEW SHEET NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION	SECURE LUMINAIRE COVER	INSTALL JUNCTION BOX COVER	TIGHTEN SIGNAL MOUNT SOCKET CONNECTIONT	REPLACE CONDUIT PLUG	REMOVE ASIM TRAFFIC COUNTER UNITS	REMOVE LUMINAIRE	FILL EROSION	REPLACE DISHED WASHER	REPLACE MISSING HANDHOLE COVER	REPLACE SAFETY CHAIN	REPLACE TRUSS MEMBER	SECURE/REPLACE VERTICAL POST CAP	TENSION CANTILEVER POST TO TRUSS CONNECTION BOLT	TIGHTEN LOOSE U-BOLT	TIGHTEN/ REPLACE HANDHOLE COVER BOLT	ALIGN LOCKING PIN	TENSION TRUSS GUSSET CONNECTION BOLT	INSTALL/REPLACE CATWALK HANDRAIL
		SHEET NUMBER				SPV.0060.24	SPV.0060.25	SPV.0060.26	SPV.0060.27	SPV.0060.28	SPV.0060.29	SPV.0060.30	SPV.0060.31	SPV.0060.32	SPV.0060.33	SPV.0060.34	SPV.0060.35	SPV.0060.36	SPV.0060.37	SPV.0060.38	SPV.0060.39	SPV.0060.40	SPV.0090.01
						EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	LF
	S-30-025	9 OF 104	I-94	NB	AT EXIT TO WEIGH STATION																		ĺ
	S-30-035	10 OF 104	I-94	SB	AT EXIT TO RUSSEL RD																		
	S-30-232	11 OF 104	STH 165	EB	AT CTH ML																		
	S-30-233	12 OF 104	STH 165 WB	WB	AT CTH ML																		
	S-30-234	13 OF 104	STH 165	WB	AT CTH EZ																		
	S-30-235	14 OF 104	STH 165 WB	WB	AT CTH EZ																		
	S-30-236	15 OF 104	STH 165	WB	APPROACHING 47TH AVE AFTER ML																		
	S-30-250	16 OF 104	STH 31	SB	AT STH 50																		
	S-30-255	17 OF 104	WEIGH STATION ENTRANCE RD	NB	WEIGH STATION ENTRANCE RD																		
	S-30-608	18 OF 104	COUNTY ML	NB	AT STH 165																		
	S-30-609	19 OF 104	COUNTY ML	SB	AT STH 165																		
BRIDGES	S-30-610	20 OF 104	COUNTY EZ	SB	AT STH 165																		
<u>8</u>	S-30-611	21 OF 104	COUNTY EZ	SB	AT STH 165																		
	S-30-612	22 OF 104	WEST FRONTAGE RD	SB	AT 71ST ST EAST OF I- 94 FRONTAGE RD																		
SIGN	S-40-009	23 OF 104	I-894	WB	AT 60TH STREET EXIT																		
#SE	S-40-018	24 OF 104	I-894	SB	I-894 @ THE HALE INTERCHANGE						2												
뿔는	S-40-049	25 OF 104	STH 100	NB	SOUTH OF USH 45 RAMP																		
~ ├─	S-40-120	26 OF 104	I-43	NB	HALE INT SPLIT WITH USH 45																		
	S-40-126	27 OF 104	I-894	SB	EXIT TO OKLAHOMA																		
₩	S-40-151	28 OF 104	1-94	WB	AT EXIT TO HAWLEY RD																		
	S-40-185	29 OF 104	STH 145 SB	EB	AT 14TH ST & WALNUT ST																		
ь ⊢	S-40-192	30 OF 104	LINCOLN MEMORIAL DR	NB	AT I-794																		
					+																		
₩ ₩	S-40-193	31 OF 104	LINCOLN MEMORIAL DR	SB	AT MICHIGAN ST																		
₫	S-40-204	32 OF 104	I-94	EB	AT 41ST ST																		
₽	S-40-228	33 OF 104	I-43	SB	AT CAPITOL DR																		
⊢	S-40-229	34 OF 104	I-43	NB	AT C & NW RAILROAD																		
<u>~</u> —	S-40-268	35 OF 104	HIGHWAY	EB	APPROACHING LAKE PKWY -																		
	S-40-269	36 OF 104	HOWARD AVE	WB	APPROACHING LAKE PKWY - STH 794																		
	S-40-283	37 OF 104	STH 32 SB	SB	COLUMBIA ADREOA CHING HILL ALCW																		
₩	S-40-285	38 OF 104	STH 145 WB	WB	APPROACHING HILLAISW INTERCHANGE																		
	S-40-301	39 OF 104	STH 794 S	SB	NORTH OF E RUSSEL AVE																		
	S-40-306	40 OF 104	GOOD HOPE RD	WB	GOOD HOPE																		
# ⊢	S-40-326	_	USH 41 SB-STH 175 SB	SB	AT USH 41																		
Œ	S-40-327	42 OF 104	USH 41	NB	AT MILL RD																		—
-	S-40-328	43 OF 104	STH 145 NB	NB	AT PARK PLACE	1																	<u> </u>
_	S-40-352	44 OF 104	USH 45	SB	AT HAMPTON OFF RAMP	1																	<u> </u>
-	S-40-355	45 0F 104	USH 45	SB	AT NORTH AVE WEST	1																	
	S-40-393	46 OF 104	I-43	SB	LAPHAM/MITCHELL ST					4													
	S-40-423	47 OF 104	GOOD HOPE RD	EB	AT USH 41/45 ON RAMP, PRIOR TO RAMP				1														
	S-40-425	48 OF 104	HIGHWAY	EB	BEFORE STH 100																		
-	S-40-426	49 OF 104	STH 175 SB	SB	BEFORE HWY 41																		
	S-40-436	50 OF 104	I-894	EB	BLANK OUT SIGNS AT TUNNEL - X & DOWN ARROW																		
-	S-40-437	51 OF 104	I-894	EB	BLANK OUT SIGNS IN ADV OF TUNNEL - X & DOWN ARROW																		
_	S-40-438	52 OF 104	I-94	NB	S-W CONNECTOR BLANK OUT SIGNS AT TUNNEL																		-
-	S-40-443	53 OF 104	1-94	WB	EAST OF 68TH ST					4													
	S-40-502	54 OF 104	I-43	NB	SOUTH OF STH 145 EXIT					6													-
-	S-40-503	55 OF 104	I-43	NB	OFF RAMP TO MCKINLEY AVE	1																	
L	S-40-512	56 OF 104	I-43	SB	1/4 MILE NORHT OF I-94					4													

REFER TO SHEET 3.4 FOR A CONTINUATION OF REPAIRS FOR THESE SIGN BRIDGES

PROJECT NO: 1000-20-68 HWY: VARIOUS COUNTY: SE REGION WIDE MISCELLANEOUS QUANTITIES SHEET 3.2 OF 3.4 E

REFER TO SHEET 3.1 FOR A CONTINUATION OF REPAIRS FOR THESE SIGN BRIDGES

STRUCTURE NUMBER	ELEVATION VIEW SHEET NUMBER	HIGHWAY	DIRECTION OF TRAVEL	LOCATION	SIGN STRUCTURE ANCHOR ROD REPAIR	REMOVE GROUT	REPLACE RODENT SCREEN	TENSION ANCHOR RODS	REPAIR FOUNDATION	REMOVE FILL/DEBRIS AND REGRADE	SPLICE CONNECTION BOLT REPLACEMENT	TIGHTEN/REPLACE POST TO MAST ARM CONNECTION	SECURE/REPLACE CHORD CAP	REINSTALL TRUSS	TENSION TRUSS CONNECTION BOL	TENSION SADDLE POST TO TRUSS/CHORD CONNECTION BOLT	CLEAN NESTING DEBRIS	INSTALL U-BOLT	INSTALL VERTICAL SIGN SUPPORT BRACKET/SUPPORT BOLTS	REPLACE SIGN BRIDGE ID PLAQUE	REPLACE SIGN PANEL CONNECTOR	REATTACH TYPE-II SIGN	REPLACE TYPE-I SIGN	INSTALL OVERHEAD SIGN MOUNTING SYSTEM	REPLACE SIGN CONNECTION ASSEMBLY	SLOTTED VERTICAL SIGN SUPPORT REPAIR	INSTALL VERITCALS SIGN SUPPORT	SECURE HANDHOLE COVER
	SHEET NUMBER				SPV.0060.01 EACH	SPV.0060.02 EACH	SPV.0060.03 EACH	SPV.0060.04 EACH	SPV.0060.05 EACH	SPV.0060.06 EACH	SPV.0060.07 EACH	SPV.0060.08 EACH	SPV.0060.09 EACH	SPV.0060.10 EACH	SPV.0060.11 EACH	SPV.0060.12 EACH	SPV.0060.13 EACH	SPV.0060.14 EACH	SPV.0060.15 EACH	SPV.0060.16 EACH	SPV.0060.17 EACH	SPV.0060.18 EACH	SPV.0165.01 SF	SPV.0060.19 EACH	SPV.0060.20 EACH	SPV.0060.21 EACH	SPV.0060.22 EACH	SPV.0060.23 EACH
S-40-521	57 OF 104	l-43	NB	S-E CONNECTOR TO 794 - FOR PLANKINTON				2																				
S-40-525	58 0F 104	I-94	EB	1/10 MILE EAST OF 25TH ST	г																							
S-40-531	59 OF 104	794	EB	EXIT TO ST. PAUL AVE - JAMES LOVELL ST																			20					
S-40-537	60 OF 104	I-794	EB	PLANKINTON AVE EXIT TO JACKSON ST															2									
S-40-549	61 OF 104	I-43	NB	1/2 MILE SOUTH OF NATIONAL AVE	_																							
S-40-554	62 OF 104	I-94	NB	1/4 MILE SOUTH OF AIRPORT EXIT												2					2							
S-40-555	63 OF 104	I-94	NB	COLLECTOR DIST, STH 119 AIRPORT EXIT							4																	
S-40-581	64 OF 104	l-894	EB	W-S RAMP				1																				
S-40-585	65 OF 104	l-94	SB	MITCHELL INTERCHANGE W-S RAMP							4																	
S-40-834	66 OF 104	I-894	WB	AT LOOMIS RD				2																				
S-40-844	67 OF 104	STH 145	SB	AT GRANTOSA DRIVE				1		1											1							1
S-40-903	68 OF 104	STH 175 NB	NB	ON RAMP TO USH 45 RAMP METERS				1																				
S-40-907	69 OF 104	STH 241 SB	SB	AT RAWSON						1																		
S-40-918	70 OF 104	STH 181	NB	AT STH 100 ENTRANCE RAMP																1								
S-40-926	71 OF 104	COUNTY PP E	EB	AT I- 43 APPROACHING CTH W	И																					8		
S-45-001	72 0F 104	I-43	SB	AT EXIT TO STH 33							12																	
S-45-002	73 OF 104	l-43	NB	1/2 MILE SOUTH OF STH 57							12																	
S-45-003	74 0F 104	I-43	SB	1/2 MILE NORTH OF STH 33 EXIT							12																	
S-45-004	75 OF 104	I-43	NB	SOUTH OF STH 57/L 43 SPLIT	Т						12																	
S-45-201	76 OF 104	STH 57 EB	EB	AT L- 43 RAMP							2			1						1								
S-45-600	77 OF 104	PORT WASHINGTON RD	SB	AT MEQUON RD																1								
S-51-008	78 0F 104	STH 38-NORTHWESTERN	WB	RAPIDS DR ON STH 38			3	1			3																	
S-51-009		STH 38 NB-NORTHWEST AVE	WB	JUST EAST OF RAPIDS DR				2			1																	
S-51-010		STH 38 SB-NORTHWEST AVE	EB	JUST EAST OF RAPIDS DR				2														1						
S-51-011	81 OF 104	STH 38 SB-NORTHWEST	EB	1/4 WEST OF CTH MM				2			1																	
S-51-214	82 OF 104	STH 20	WB	AT RENAISSSANCE BLVD				1																				
S-64-012	83 OF 104	USH 12	WB	AT COUNTY NN							2				2													
S-64-017	84 OF 104	USH 12	SB	ON C-D ROAD TO L- 43 NB	1												1											
S-66-201	85 OF 104	PARADISE DR EB	EB	RAMPS AT USH 45								1													1			
S-67-215	86 OF 104	COUNTY T	NB	OVERPASS I-94				1																1				
S-67-216	87 OF 104	COUNTY T	SB	OVERPASS I-94																				1				
S-67-230	88 0F 104	STH 164	NB	SOUTH OF CTH ES	1			1				1																
S-67-234	89 OF 104	STH 59 WB	WB	AT BARKER						1																		
S-67-236	90 OF 104	COUNTY Y	NB	AT GREENFIELD				1																				
S-67-238	91 OF 104	STH 145 SB	SB	1/8 MILE NORTH OF US 41/45	5 1			1																				
S-67-243	92 OF 104	STH 145 SB	SB	NORTH INTERCHANGE				1																				
S-67-900	93 OF 104	I-94	EB	EXIT RAMP TO CTH P																						5		
S-67-901	94 OF 104	I-94	WB	EXIT RAMP TO CTH P AT ENTRANCE RAMPS TO I-				2								-			-	-						6		
S-67-902	95 OF 104	COUNTY P	SB	94		-	1									1										6		
S-67-903	96 OF 104	COUNTY P	NB	AT ENTRANCE RAMPS TO I-		-	-									1			-	-						6		
S-67-904	97 OF 104	COUNTY P	SB	SB AT ENTRANCE RAMPS TO I- 94		1	1	1								1			-							6		
S-67-905	98 OF 104	PABST FARD RD	EB	APPROACHING CTH P			-									-										4		
S-67-909	99 OF 104	STH 67	SB	AT CTH B								1																
S-67-929	100 OF 104	STH 83	NB	AT CTH L				2			1					-				1						4		
S-67-930	101 OF 104	STH 83	SB	AT CTH L		1	1	2								1				1						4		
S-67-931	102 OF 104	STH 83	SB	AT CTH X				2												1						5		
UNDISTRIBUTED																												
TOTAL				+	11	4	9	63	1	5	93	7	1	1	3	2	1	3	2	10	61	2	20	2	1	87	7	1
TOTAL					11	4	9	63	1	5	93	7	1	1	3	2	1	3	2	10	61	2	20	2	1	87	7	$_{\perp}$

SHEET 3.3 OF 3.4 **E** PROJECT NO: 1000-20-68 HWY: VARIOUS COUNTY: SE REGION WIDE MISCELLANEOUS QUANTITIES PLOT NAME :

REFER TO SHEET 3.2 FOR A CONTINUATION OF REPAIRS FOR THESE SIGN BRIDGES

	STRUCTURE ELEV NUMBER VI	ELEVATION	HIGHWAY	DIRECTION OF TRAVEL	LOCATION	SECURE LUMINAIRE COVER	INSTALL JUNCTION BOX COVER	TIGHTEN SIGNAL MOUNT SOCKET CONNECTIONT	PLACE CONDUIT PLUG	REMOVE ASIM TRAFFIC COUNTER UNITS	REMOVE LUMINAIRE	FILL EROSION	REPLACE DISHED WASHER	REPLACE MISSING HANDHOLE COVER	REPLACE SAFETY CHAIN	REPLACE TRUSS MEMBER	SECURE/REPLACE VERTICAL POST CAP	TENSION CANTILEVER POST TO TRUSS CONNECTION BOLT	TIGHTEN LOOSE U-BOLT	TIGHTEN/ REPLACE HANDHOLE COVER BOLT	ALIGN LOCKING PIN	TENSION TRUSS GUSSET CONNECTION BOLT	INSTALL/REPLACE CATWALK HANDRAIL
	HOMBER	SHEET NUMBER		OF THATEE		SPV.0060.24	SPV.0060.25	SPV.0060.26 S	SPV.0060.27	SPV.0060.28	SPV.0060.29	SPV.0060.30	SPV.0060.31	SPV.0060.32	SPV.0060.33	SPV.0060.34	SPV.0060.35	SPV.0060.36	SPV.0060.37	SPV.0060.38	SPV.0060.39	SPV.0060.40	SPV.0090.01
L						EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	LF
	S-40-521	57 OF 104	I-43	NB	S-E CONNECTOR TO 794 - FOR PLANKINTON		1																
	S-40-525	58 OF 104	I-94	EB	1/10 MILE EAST OF 25TH ST	Г				4													
	S-40-531	59 OF 104	794	EB	EXIT TO ST. PAUL AVE - JAMES LOVELL ST																		
	S-40-537	60 OF 104	I-794	EB	PLANKINTON AVE EXIT TO JACKSON ST																		
	S-40-549	61 OF 104	I-43	NB	1/2 MILE SOUTH OF NATIONAL AVE	-				4													
	S-40-554	62 OF 104	I-94	NB	1/4 MILE SOUTH OF AIRPORT EXIT																		
	S-40-555	63 OF 104	I-94	NB	COLLECTOR DIST, STH 119 AIRPORT EXIT																		
	S-40-581	64 OF 104	I-894	EB	W-S RAMP																		
	S-40-585	65 OF 104	I-94	SB	MITCHELL INTERCHANGE W-S																		
	S-40-834	66 OF 104	I-894	WB	AT LOOMIS RD																		
	S-40-844	67 OF 104	STH 145	SB	AT GRANTOSA DRIVE																		
	S-40-903	68 OF 104	STH 175 NB	NB	ON RAMP TO USH 45 RAMP METERS			2															
	S-40-907	69 OF 104	STH 241 SB	SB	AT RAWSON																		
s l	S-40-918	70 OF 104	STH 181	NB	AT STH 100 ENTRANCE RAMP	,																	
岁	S-40-926	71 OF 104	COUNTY PP E	EB	AT I- 43 APPROACHING CTH N	v																	
BRIDGE	S-45-001	72 OF 104	I-43	SB	AT EXIT TO STH 33																		
8 T	S-45-002	73 OF 104	I-43	NB	1/2 MILE SOUTH OF STH 57																		
S	S-45-003	74 OF 104	I-43	SB	1/2 MILE NORTH OF STH 33 EXIT																		
ESE	S-45-004	75 OF 104	I-43	NB	SOUTH OF STH 57/I 43 SPLIT																		
Ė	S-45-201	76 OF 104	STH 57 EB	EB	AT I- 43 RAMP																		
E	S-45-600	77 OF 104	PORT WASHINGTON RD	SB	AT MEQUON RD																		
SS	S-51-008	78 OF 104	STH 38-NORTHWESTERN	WB	RAPIDS DR ON STH 38																		
PA	S-51-009	79 OF 104	STH 38 NB-NORTHWEST AVE	WB	JUST EAST OF RAPIDS DR																		
\footnote{\pi}	S-51-010	80 OF 104	STH 38 SB-NORTHWEST AVE	EB	JUST EAST OF RAPIDS DR																		
누	S-51-011	81 OF 104	STH 38 SB-NORTHWEST	EB	1/4 WEST OF CTH MM																		
	S-51-214	82 OF 104	STH 20	WB	AT RENAISSSANCE BLVD																		
TA	S-64-012	83 OF 104	USH 12	WB	AT COUNTY NN																		
	S-64-017	84 OF 104	USH 12	SB	ON C-D ROAD TO I- 43 NB																		
5 H	S-66-201	85 OF 104	PARADISE DR EB	EB	RAMPS AT USH 45																		
∢ -	S-67-215	86 OF 104	COUNTY T	NB	OVERPASS I-94																		
유	S-67-216	87 OF 104	COUNTY T	SB	OVERPASS I-94																		
m	S-67-230	88 OF 104	STH 164	NB	SOUTH OF CTH ES																		
	S-67-234	89 OF 104	STH 59 WB	WB	AT BARKER																		
単上	S-67-236	90 OF 104	COUNTY Y	NB	AT GREENFIELD																		
15	S-67-238	91 OF 104	STH 145 SB	SB	1/8 MILE NORTH OF US 41/45	5																	
£ -	S-67-243	92 OF 104	STH 145 SB	SB	NORTH INTERCHANGE																		
出上	S-67-900	93 OF 104	I-94	EB	EXIT RAMP TO CTH P																		
~ H	S-67-901	94 OF 104	1-94	WB	EXIT RAMP TO CTH P																		
	S-67-902	95 OF 104	COUNTY P	SB	AT ENTRANCE RAMPS TO I-																		
	S-67-903	96 OF 104	COUNTY P	NB	AT ENTRANCE RAMPS TO I-																		
-	S-67-904	97 OF 104	COUNTY P	SB	SB AT ENTRANCE RAMPS TO																		
-	S-67-905	98 OF 104	PABST FARD RD	EB	I- 94 APPROACHING CTH P																		
	S-67-909	99 OF 104	STH 67	SB	AT CTH B																		
-	S-67-929	100 OF 104	STH 83	NB	AT CTH L																		
\vdash	S-67-930	101 OF 104	STH 83	SB	AT CTH L																		
\vdash	S-67-931	102 OF 104	STH 83	SB	AT CTH X																		
		1	2 65	35	9 %																		
	UNDISTRIBUTED											2	4	2	2	1	2	2	4	2	2	2	4
	TOTAL					4	1	2	1	26	2	2	4	2	2	1	2	2	4	2	2	2	4

PROJECT NO: 1000-20-68

HWY: VARIOUS

PLOT BY: Veronica Chavezv8I

COUNTY: SE REGION WIDE

MISCELLANEOUS QUANTITIES

PLOT NAME :

SHEET 3.4 OF 3.4 **E**

PLOT SCALE: \$\$.....plo†scale.....\$\$ wisdot/cadds SHEET 43

3

l.plt CEI_TextSub_ScreenControlByLevel.tbl

3

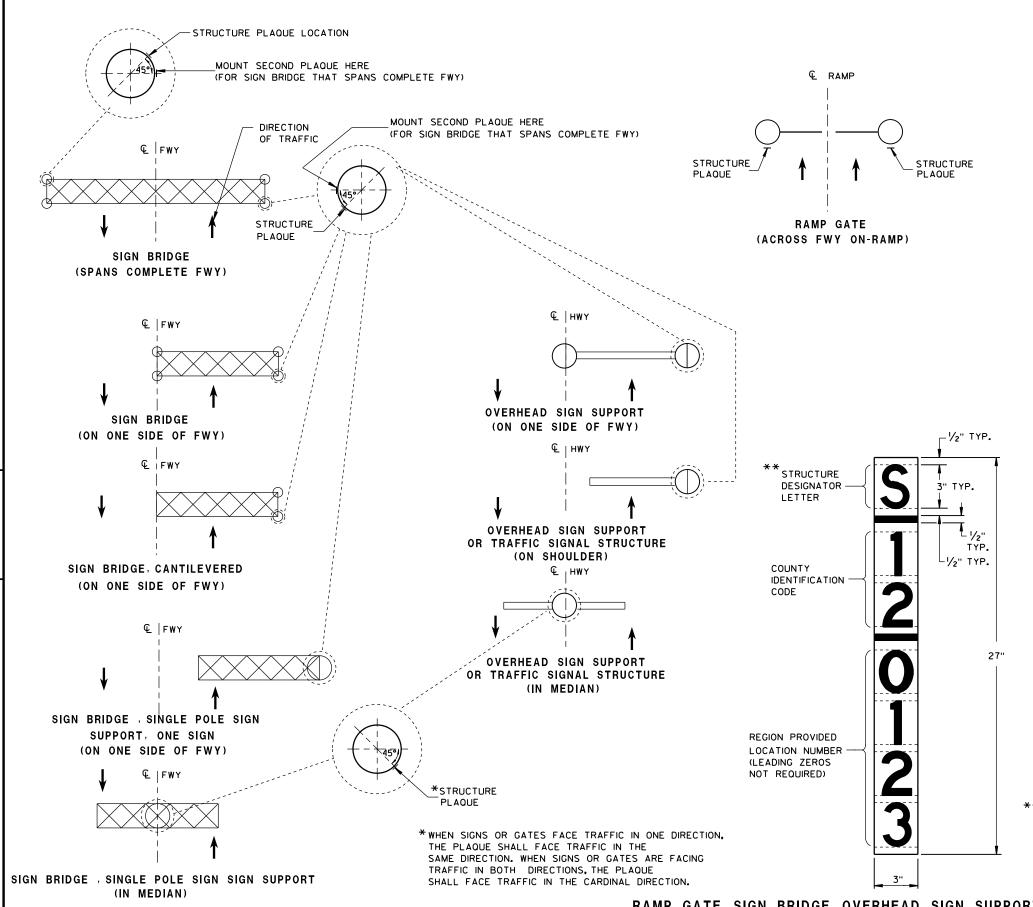
Standard Detail Drawing List

12A04-03 15D14-02	STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES & OVERHEAD SIGN SUPPORTS & TRAFFIC SIGNALS TRAFFIC CONTROL, TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY, SHORT-TERM (LESS THAN 24 HOURS)
15D14-03	TRAFFIC CONTROL, TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY, SHORT-TERM (LESS THAN 24 HOURS)
15D15-01 15D16-02	TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D20-03 15D22-02	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY TRAFFIC CONTROL. TWO LANE CLOSURE. NON-FREEWAY/EXPRESSWAY
15D22-02 15D27-02	TRAFFIC CONTROL, TWO LANE CLOSURE, NON-FREEWAY/EXPRESSWAY TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH
15D28-02	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

6



3.D.D. 12 A 4-3



6

Ö

12

 \triangleright

LOCATION OF RAMP GATE, SIGN BRIDGE, OVERHEAD

SIGN SUPPORT & TRAFFIC SIGNAL STRUCTURE PLAQUES

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN IN THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PLAQUES SHALL BE INCIDENTAL TO ALL NEW INSTALLATIONS.

IF THE PROPOSED SIGN BRIDGE OR OVERHEAD SIGN SUPPORT IS REPLACING AN EXISTING SIGN BRIDGE OR OVERHEAD SIGN SUPPORT, A NEW IDENTIFICATION PLAQUE WILL BE REQUIRED.

FASTEN TOP, CENTER AND BOTTOM OF PLAQUE TO POLE OR OTHER LOCATION AS FOLLOWS:

GALVANIZED STEEL SHAFT - 3 STAINLESS STEEL POP RIVETS

A588 STEEL SHAFT - SHIM FOR DRAINAGE WITH STAINLESS WASHERS; FASTEN WITH STAINLESS SELF-TAPPING SCREWS

ALUMINUM SHAFTS - 3 ALUMINUM POP RIVETS

MOUNTING HEIGHT SHALL BE APPROXIMATELY 5.0' ABOVE CURB OR SHOULDER. ADJUST IF IT IS KNOWN THAT REQUIRED TRAFFIC SIGNS WILL OBSTRUCT.

PLAQUE MATERIALS:

BASE - SHEET ALUMINUM, 0.060" THICK.

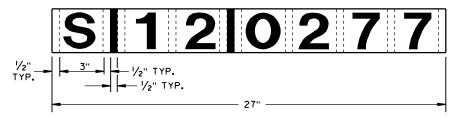
FACE - WHITE, SELF-ADHESIVE VINYL SHEETING, NON-RETROREFLECTIVE

LINES - BLACK, 1/2" WIDE, SELF-ADHESIVE

CHARACTERS:- BLACK, SELF ADHESIVE, SERIES "D", SIZE AS SHOWN.

FOR SIGN BRIDGES, STRUCTURE MOUNTED, THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY AS SHOWN ON THE DRAWING. THE STRUCTURE PLAQUE SHALL BE MOUNTED HORIZONTALLY TO THE BACK OF THE SIGN, BETWEEN THE ALUMINUM EXTRUSIONS, NEAR THE TOP LEFT HAND CORNER OF THE SIGN. THE BASE MATERIAL SHALL BE OMITTED AND THE FACE ADHERED DIRECTLY TO THE ALUMINUM SURFACE. PRIOR TO ADHERING THE MATERIAL, THE ALUMINUM SURFACE SHALL BE SMOOTH, CLEAN AND DRY.

WHERE SIGN BRIDGE ILLUMINATION IS PROVIDED, THE STRUCTURE MUST ALSO HAVE A SIGN BRIDGE CIRCUIT PLAQUE AS SHOWN IN THE ELECTRICAL DETAILS.



IDENTIFICATION PLAQUE FOR SIGN BRIDGE, STRUCTURE MOUNTED

** LETTER "G" UTILIZED FOR RAMP GATES. LETTER "S" UTILIZED FOR SIGN BRIDGES, OVERHEAD SIGN SUPPORTS, AND TRAFFIC SIGNALS.

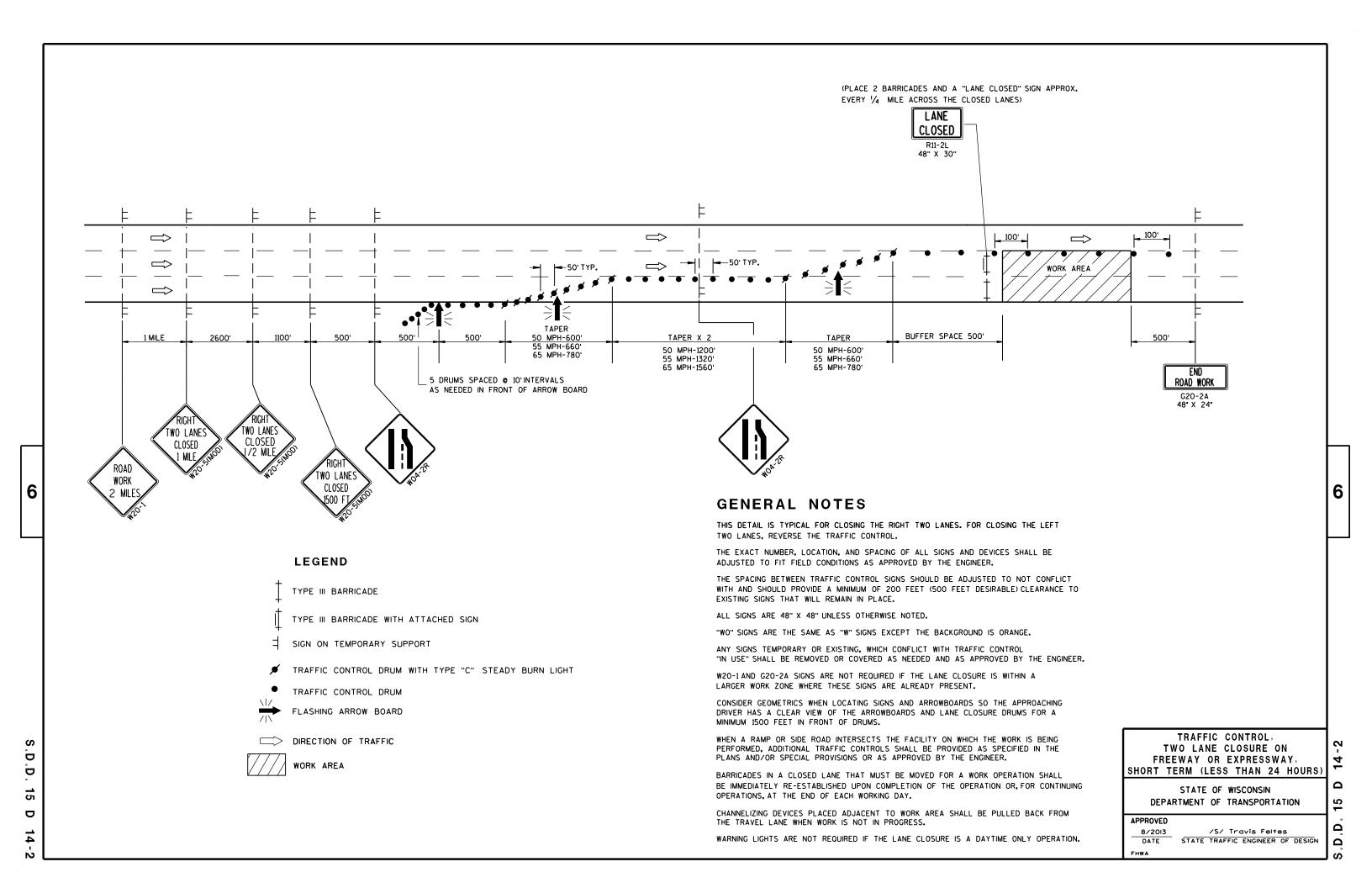
STRUCTURE IDENTIFICATION PLAQUES, RAMP GATES, SIGN BRIDGES, OVERHEAD SIGN SUPPORTS, & TRAFFIC SIGNALS

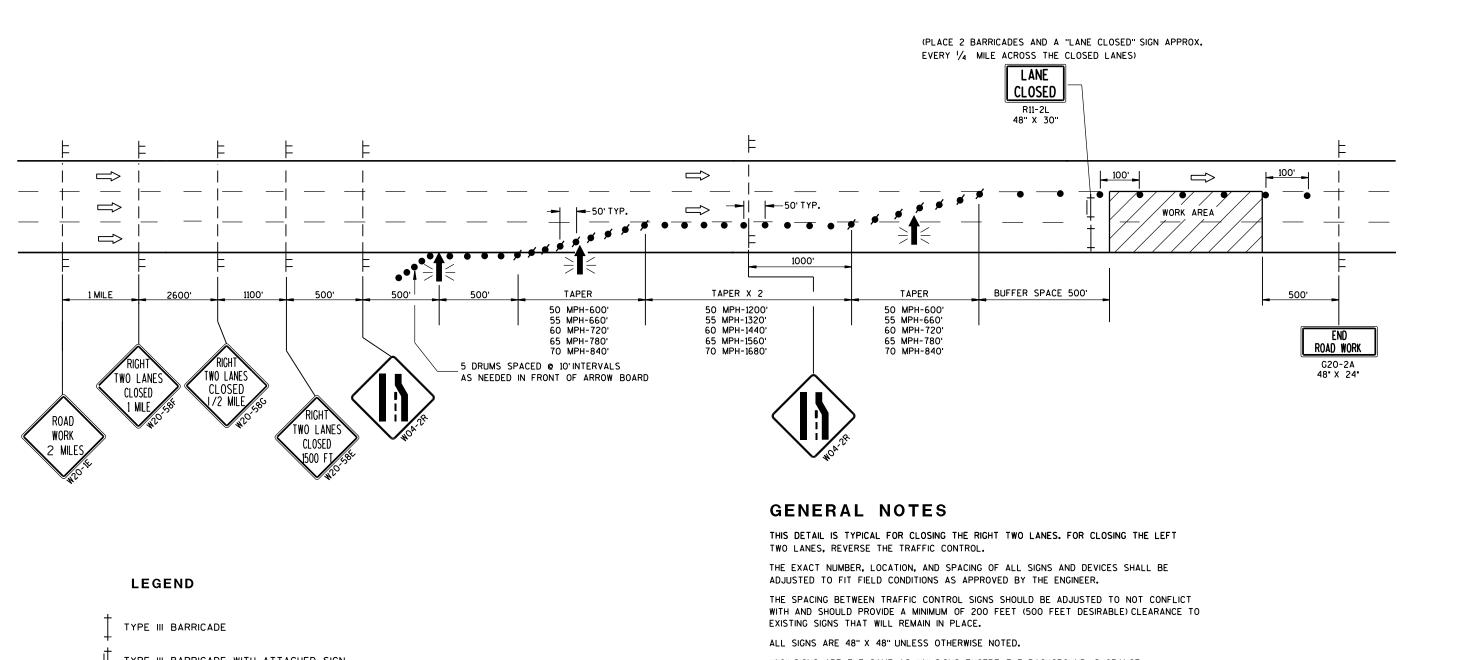
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

DATE STATE TRAFFIC ENGINEER OF DESIGN

RAMP GATE, SIGN BRIDGE, OVERHEAD SIGN SUPPORT AND TRAFFIC SIGNAL STRUCTURE PLAQUE FOR SIGN BRIDGES AND OVERHEAD SIGN SUPPORT WHICH ARE NOT STRUCTURE MOUNTED





6

TYPE III BARRICADE WITH ATTACHED SIGN

SIGN ON TEMPORARY SUPPORT

TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT

TRAFFIC CONTROL DRUM

FLASHING ARROW BOARD

DIRECTION OF TRAFFIC

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-IE AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

WHEN A RAMP OR SIDE ROAD INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

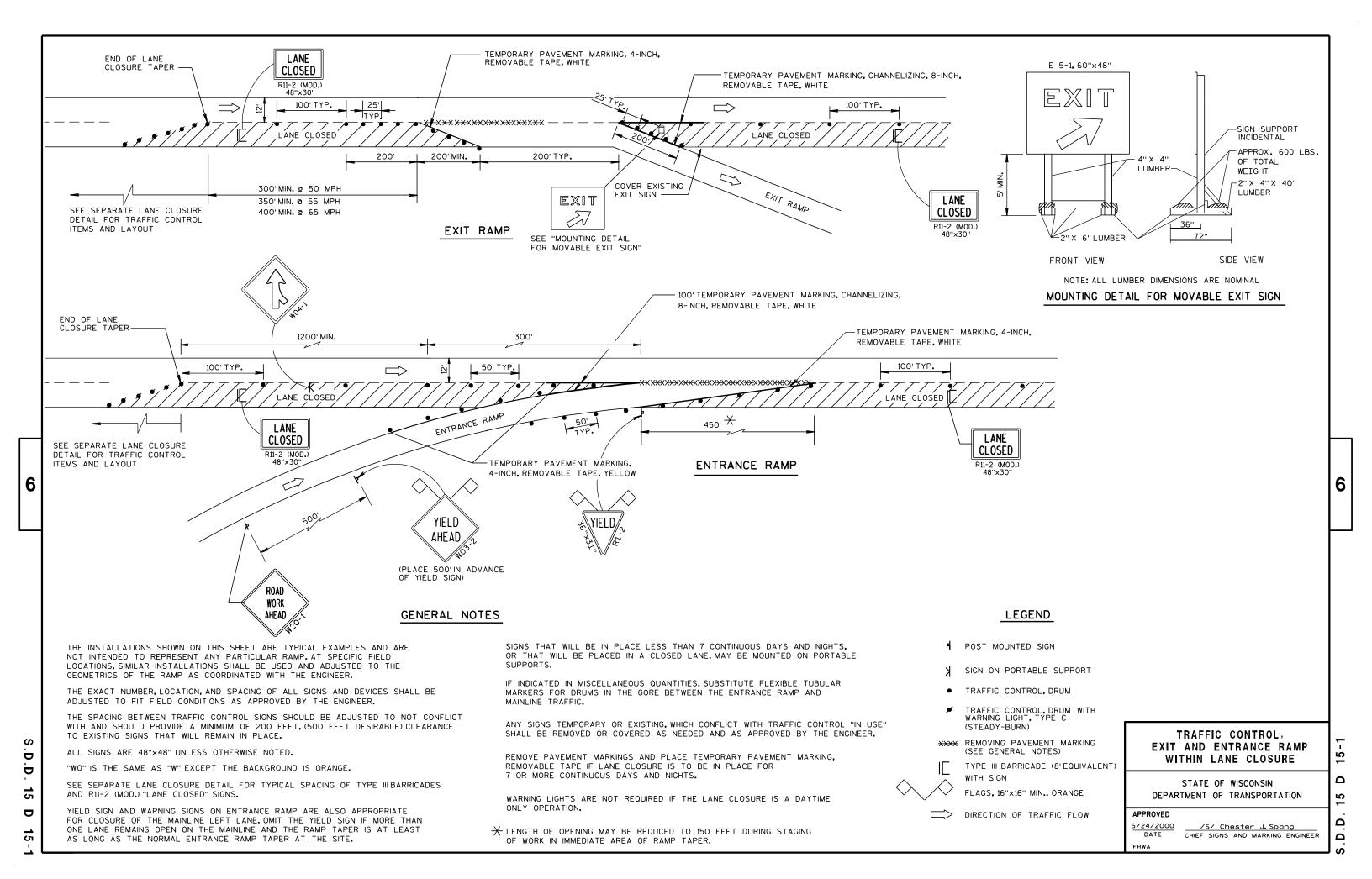
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL, TWO LANE CLOSURE ON FREEWAY OR EXPRESSWAY, SHORT TERM (LESS THAN 24 HOURS)

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

July 14, 2015 /S/ Peter Amakobe Atepe DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

Ω



LEGEND

TYPE III BARRICADE

TYPE III BARRICADE WITH ATTACHED SIGN

TRAFFIC CONTROL DRUM

SIGN ON PERMANENT SUPPORT

(△) TYPE "A" WARNING LIGHT (FLASHING)

DIRECTION OF TRAFFIC

16

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

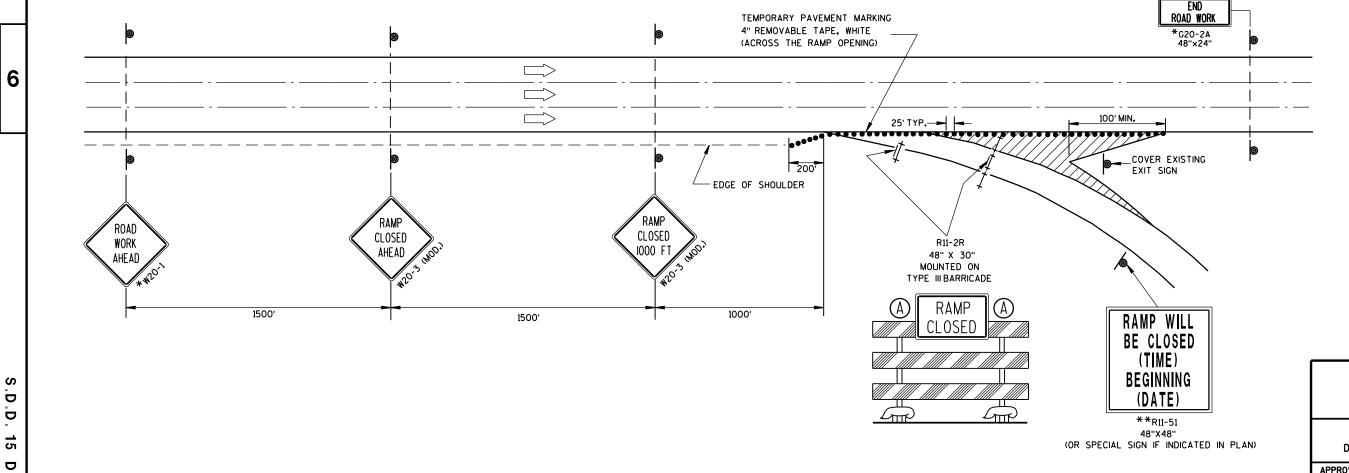
PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF RAMP CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROPOFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN THE WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12-HOUR DURATION.

*W20-1AND G20-2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

** PLACE "RAMP WILL BE CLOSED" SIGN 10 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



TRAFFIC CONTROL, EXIT RAMP CLOSURE

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED 8/2013

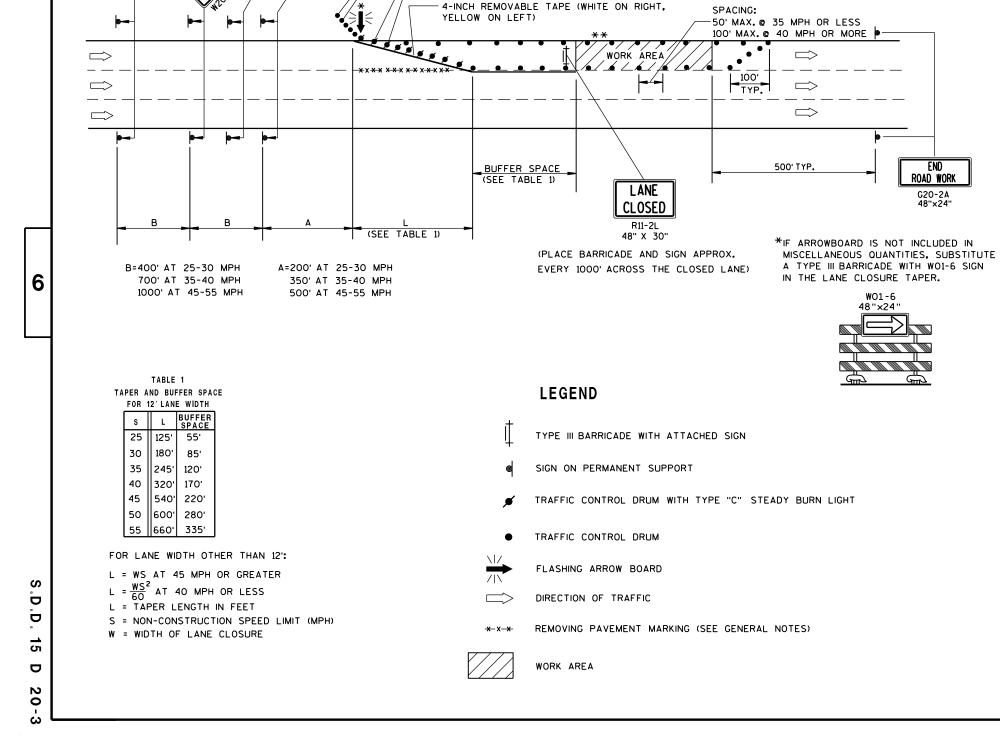
B/2013

DATE

STATE TRAFFIC ENGINEER OF DESIGN
FHWA

D.D. 15 D 16





(5) DRUMS SPACED @ 10'

INTERVALS AS NEEDED IN

FRONT OF ARROW BOARD

25'@ 35 MPH OR LESS

50'@ 40 MPH OR MORE

TEMPORARY PAVEMENT MARKING.

SPACING:

ROAD WORK

NEXT___MILES

G20-1

60" X 24"

CLOSED

AHEAD

AHEAD

GENERAL NOTES

**THE LINE OF DRUMS SHOWN ALONG THE MEDIAN/CENTERLINE

ADJACENT TO THE WORK AREA. FOR THIS CONDITION INSTALL

W20-1 "ROAD WORK AHEAD" SIGN FOR OPPOSING DIRECTION OF

IS REQUIRED ONLY WHERE THERE IS OPPOSING TRAFFIC

TRAFFIC, IN ADVANCE OF THE WORK AREA.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W2O-1, G2O-1 AND G2O-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

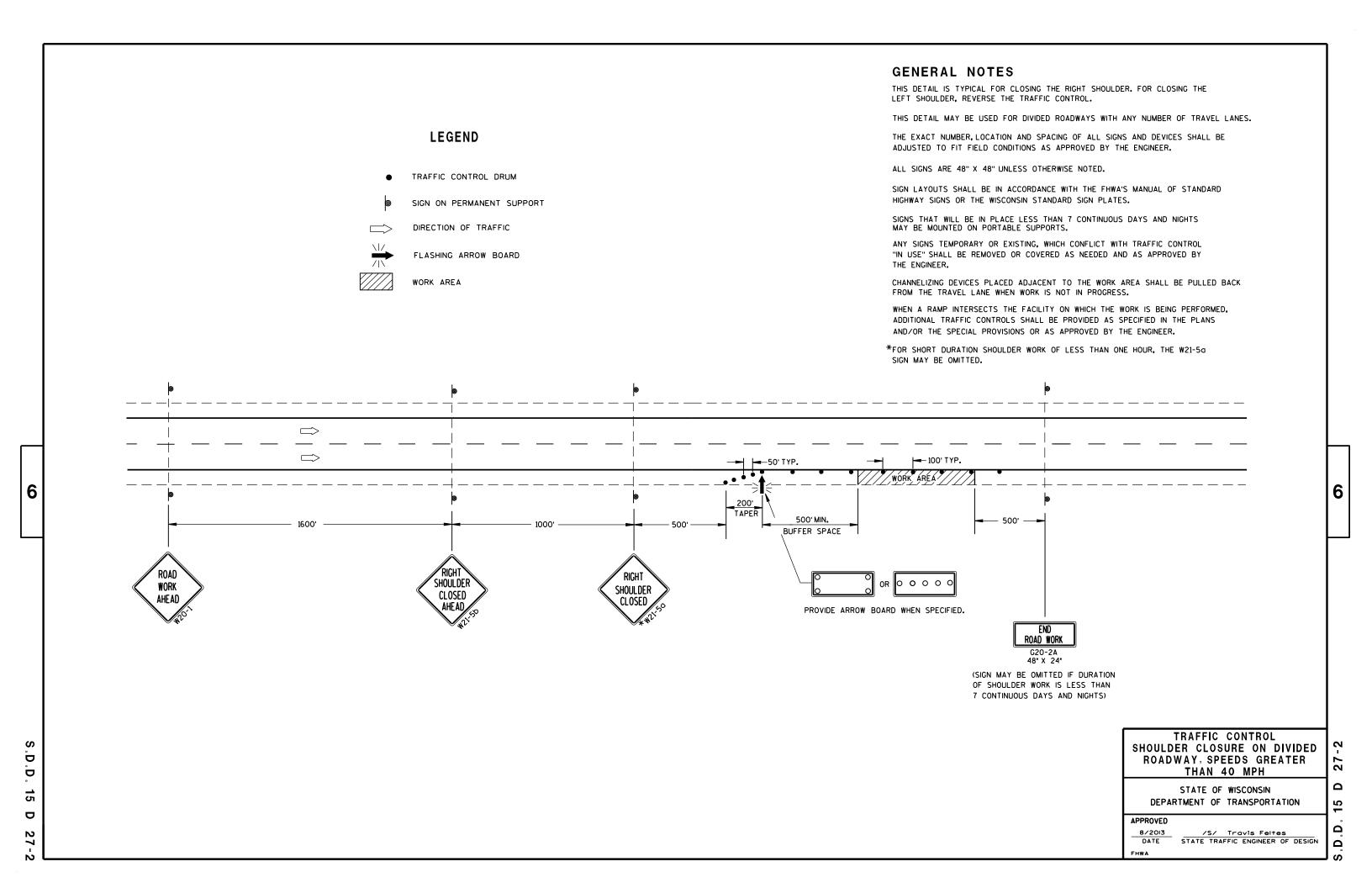
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

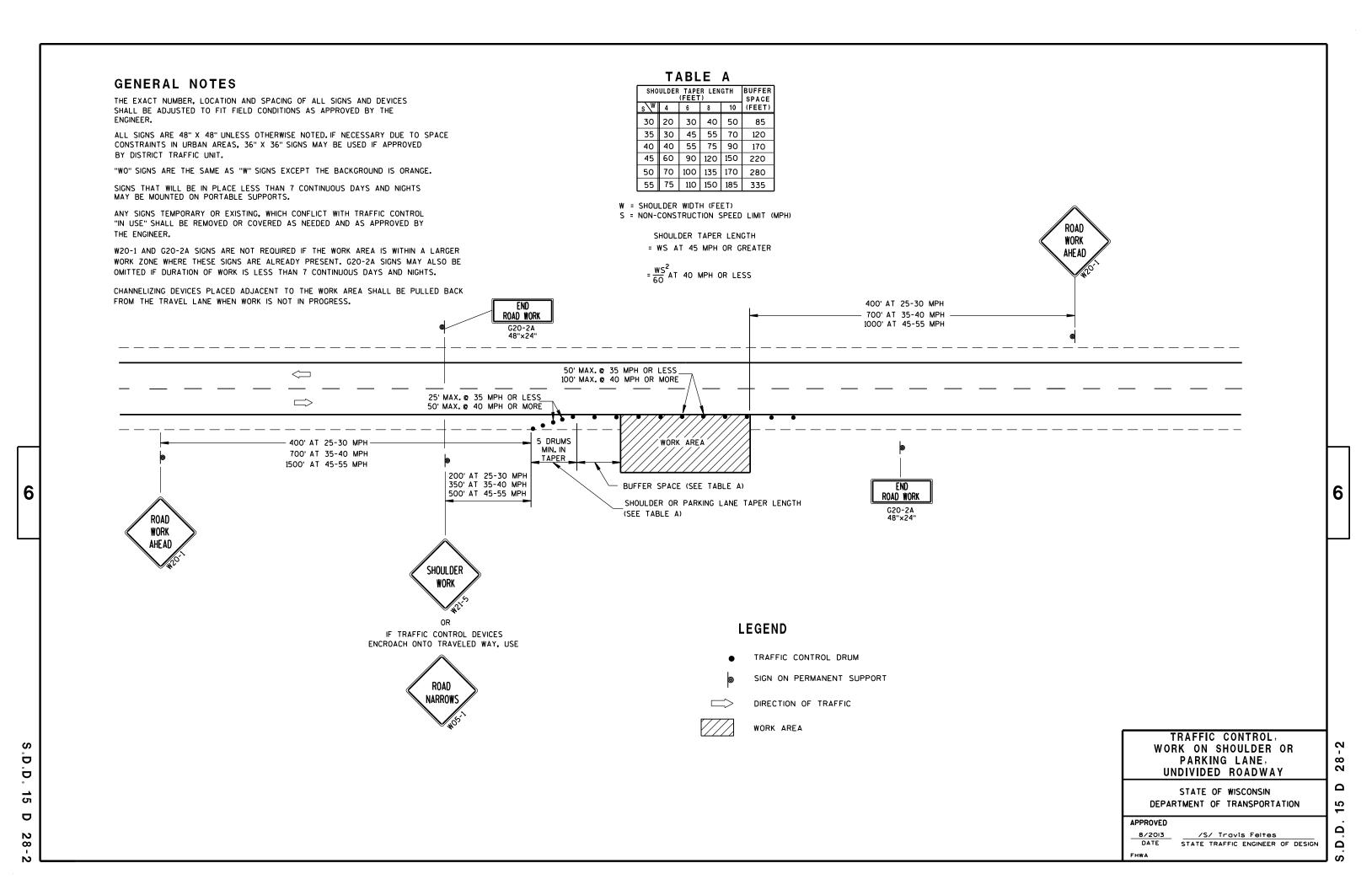
TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Feb. 2015
DATE
STATE TRAFFIC ENGINEER OF DESIGN

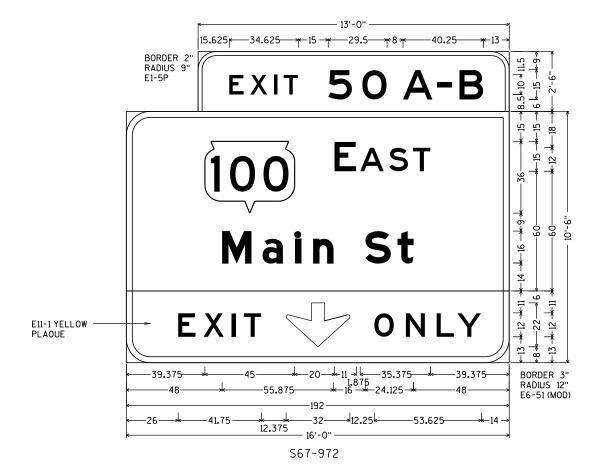
S.D.D. 15 D 2





GENERAL NOTES:

- 1. DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE PLANS.
- 2. UNLESS OTHERWISE NOTED, ALL SIGNS SHOWN ON THIS SHEET ARE "SIGNS, TYPE $\;$ I".
- 3. UNLESS OTHERWISE NOTED, TYPE II SIGNS ON
 THIS SHEET SHALL HAVE "TYPE H REFLECTIVE SHEETING" AND,
 "TYPE H MESSAGE MATERIAL". TYPE I SIGNS SHALL HAVE TYPE SH
 REFLECTIVE SHEETING.
- 4. UNLESS OTHERWISE NOTED, ALL SIGNS SHOWN ON THIS SHEET SHALL HAVE A GREEN BACKGROUND AND WHITE MESSAGE.
- 5. TYPE IISIGNS ALL UPPER CASE MESSAGE (EXCEPT ON SHIELDS OR WHERE OTHERWISE NOTED) SHALL BE "SERIES E. ALL LOWER CASE MESSAGE WITH AN INITIAL UPPER CASE LETTER SHALL BE "SERIES E.
- 6.TYPE ISIGNS ALL
 UPPER CASE MESSAGE (EXCEPT ON SHIELDS OR WHERE OTHERWISE
 NOTED) SHALL BE SERIES "E" MODIFIED. ALL LOWER CASE
 MESSAGE WITH AN INITIAL UPPER CASE LETTER SHALL BE SERIES "E" MODIFIED.
 ALL CAP WORDS ARE "SERIES E"
- 7. UNLESS OTHERWISE NOTED, ALL SIGNS SHOWN ON THIS SHEET SHALL HAVE "TYPE A" OR "TYPE C" ARROWS AS SHOWN. SEE THE STANDARD SIGN PLATES FOR FURTHER DETAILS.
- 8. SEE THE STANDARD SIGN PLATES FOR FURTHER DETAILS ON ROUTE MARKER SHIELDS.
- 9. THE SIGN NUMBER IS DENOTED IN THE CIRCLE NEAR EACH DETAIL.
- 10. NUMBER FRACTIONS FOR INTERCHANGE SEQUENCE SIGNS SHALL BE SERIES "E" PER PLATES A11-7 AND A11-10
- 11. DO NOT SCALE.



SHEET 1 OF 1

SHEET

PROJECT NO:1000-20-68 HWY: VARIOUS COUNTY: SE REGION WIDE SIGNING DETAIL - TYPE I SIGNS

FILE NAME : J:\projects\d2_10002068\Sign\070101_ms.dgn

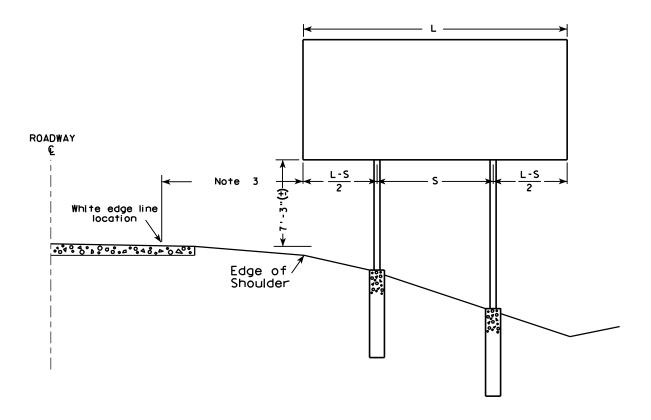
PLOT DATE: 24-NOV-2015 08:23

PLOT NAME :

PLOT BY : dotshw

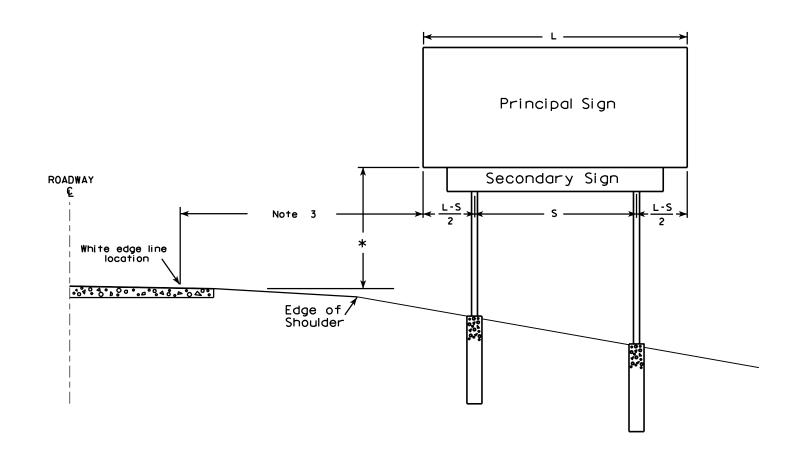
PLOT SCALE: 48.125:1

SCALE:



GENERAL NOTES

- 1. For a 2 post installation, S equals 3L/5, but shall not be less than 9 ft.
- 2. For a 3 post installation, S equals 5L/7, but shall not be less than 18 ft., and the space between any two posts shall not be less than 9 ft.
- 3. Unless noted in the plan, the sign offset distance shall be a minimum of 17'-6", desirable 30'-0".
- 4. The (+) tolerance shown on this sheet is 3 in.
- 5. The vertical sign height clearance detailed is measured from the bottom of the sign to the near edge of pavement.
- 6. Post lengths shown in the miscellaneous quantities are estimated lengths. The contractor shall verify post lengths at the time of final grading.
- 7. Refer to the Traffic Guidelines Manual for further guidance on minimum vertical clearance requirements.



* Clearance is $8'-3''(\pm)$ when the secondary sign is 3 ft. or less in height. For secondary signs larger than 3 ft., the clearance to the bottom of the secondary sign shall be $5'-3''(\pm)$.

> TYPICAL INSTALLATION OF TYPE I SIGNS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED for State Traffic Engineer PLATE NO. <u>A4-1.9</u>

DATE 4/02/08

SHEET NO:

PROJECT NO:

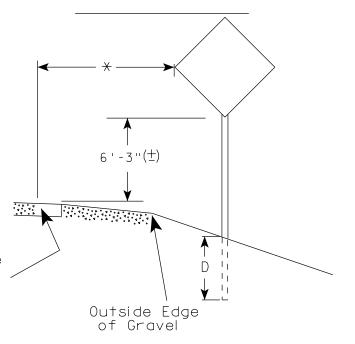
PLOT DATE: 02-APR-2008 15:49

PLOT BY : ditjph

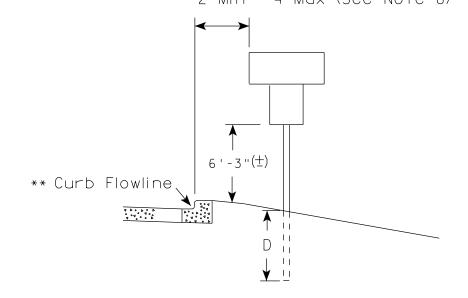
URBAN ARFA

2' Min - 4' Max (See Note 6) 7'-3"(士) ** Curb Flowline. White Edgeline Location

RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



5'-3"(生) A POLICE AND A POL D^{-1} Outside Edae of Gravel

White Edgeline

Location

COUNTY:

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

(Min) (Sq.Ft.) 20 or Less 4' Greater than 20

PLOT BY : mscj9h

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

GENERAL NOTES

- 1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is $7'-3''(\pm)$ or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A2-1S) is $7'-3''(\pm)$ or $6'-3''(\pm)$ per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is 5' - 3'' (\pm).
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (+) tolerance for mounting height is 3 inches.
- 8. Folding signs shall be mounted at a height of 5'-3'' (\pm) or as directd by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm) . The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3'' (\pm).

POST EMBEDMENT DEPTH

D

Area of Sign

Installation

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

for State Traffic Engineer

DATE 7/23/15

PLATE NO. <u>A4-3.20</u>

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A43.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:21

PLOT NAME :

PLOT SCALE: 99.237937:1.000000



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

GENERAL NOTES

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways, mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3'' (±) or 6'-3'' (±) per urban or rural detail respectively.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).
- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- *** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

URBAN AREA RURAL AREA (See Note 3) 2'Min - 4'Max (See Note 6) ₩E# FF# 6'-3"(±) 6'-3"(±) 7'-3"(±) ** Curb ****\ Flowline D **7000** White Edgeline D 11 White Edgeline, Location Outside Edae Location

2'Min - 4'Max (See Note 6) 6'-3"(±) Curb Flowline. - 11

48" DIAMOND WARNING SIGN

HWY:

_ 26" 5 ' - 3 "(±) White Edgeline Location Outside Edge of Gravel 48" DIAMOND WARNING SIGN

COUNTY:

Outside Edge

of Gravel

	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRED		
	L	E	
* * *	Greater than 48" Less than 60"	12"	
	60" to 120"	L/5	l

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN (FOUR POSTS REQUIRE	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

of Gravel

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

Matther

SHEET NO:

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 23-JUL-2015 15:23

PLOT SCALE: 107.021305:1.000000

WISDOT/CADDS SHEET 42

PLOT NAME :

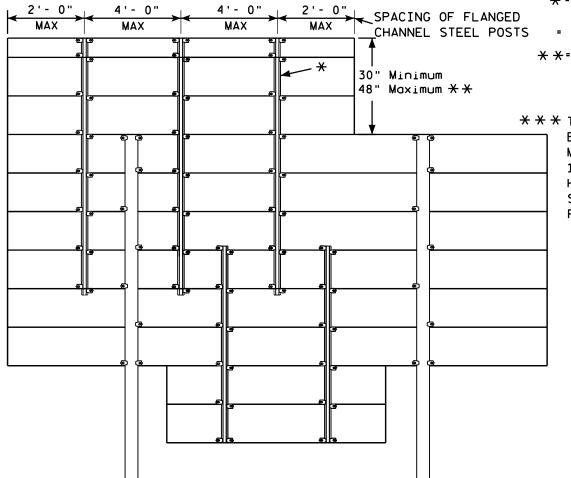
PLOT BY: mscj9h

WISCONSIN DEPT OF TRANSPORTATION APPROVED

For State Traffic Engineer

PLATE NO. 44-4.14 DATE 7/23/15





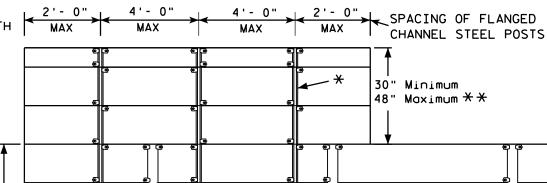
*=2.00 lb/ft FLANGED CHANNEL, MIN. YIELD STRENGTH

CHANNEL STEEL POSTS = 60,000 PSI (GRADE 60) GALVANIZED

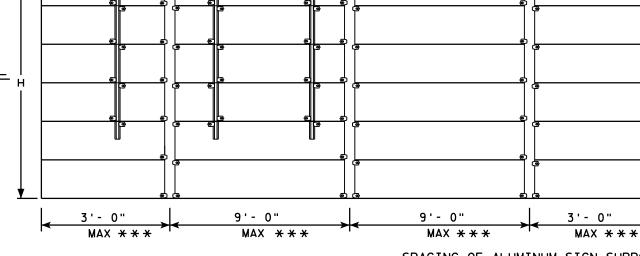
SIGN BRIDGE MOUNTED SIGN

* *= FOR 48" HEIGHT PANELS ON OVERHEAD STRUCTURES, ENTIRE SIGN SHALL BE CENTERED VERTICALLY ABOUT THE DEPTH OF THE TRUSS.

* * THESE SPACING DISTANCES SHALL ONLY BE USED WHEN THE MAIN SIGN HAS A MAXIMUM HEIGHT (DIMENSION H) OF 16 FT OR LESS. FOR SIGNS WITH A HEIGHT OF GREATER THAN 16 FT, STRUCTURAL CALCULATIONS SHALL BE PERFORMED.



FLANGE CHANNEL DETAIL 1/₄ → NOT TO SCALE



SPACING OF ALUMINUM SIGN SUPPORTS 5" X 3.5" X 3.7 LBS./ft.

GENERAL NOTES

- 1. Flanged channel steel posts shall conform to size and material above, and shall be considered as incidental to other items in the contract.
- 2. Number of Flanged channel steel supports varies with length of panel and shall be spaced as shown:

PANEL LENGTH 8'-0" OR LESS = 2 CHANNELS PANEL LENGTH 9'- 0" - 12'- 0" = 3 CHANNELS PANEL LENGTH 13'- 0" OR MORE = 4 CHANNELS

If the flanged channel steel posts can not be horizontally spaced as shown, they can be moved so as to securely hold the sign.

3. The EXIT NUMBER PANEL shall normally be positioned above the guide sign aligned with the right edge of the guide sign. If the guide sign indicates a left exit, the EXIT NUMBER PANEL shall be aligned with the left edge of the guide sign.

2'- 0"

- 4. If the bolt holes in the top panel (EXIT NUMBER), or sub panel (NEXT EXIT) line up with holes in main sign panel, stitch bolts shall be used in addition to the channels.
- 5. Provide post clips for each sign as shown. (Please note the differences between a ground mounted versus Sign bridge mounted sign as far as number of clips required on the main supports or beams)
- 6. Structural steel sign supports shall extend to the top of the main signs, as shown on the above details.

ATTACHMENT OF GUIDE SIGNS TO SUPPORTS

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 12/05/13

PLATE NO. A4-6.12

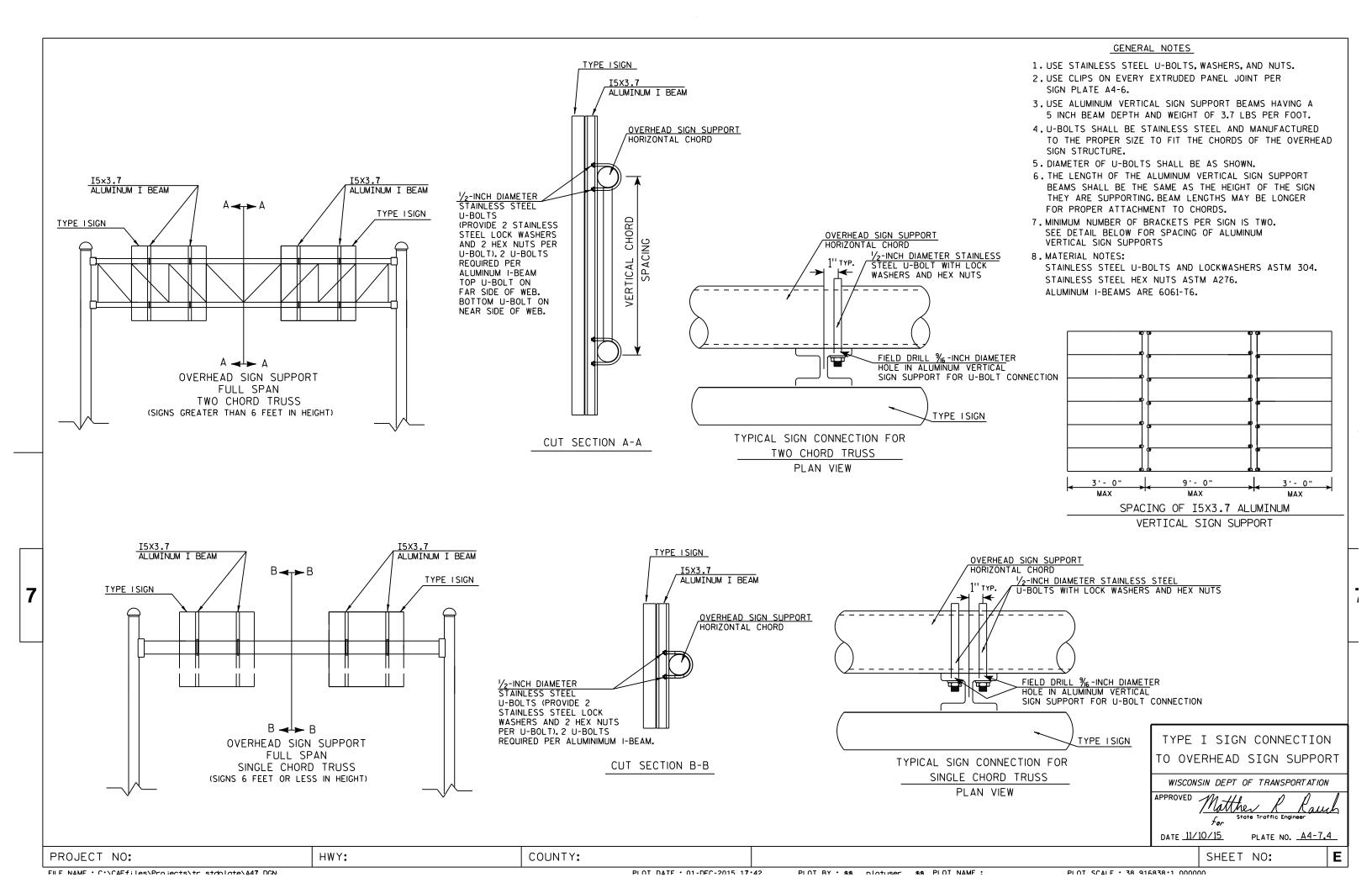
SHEET NO:

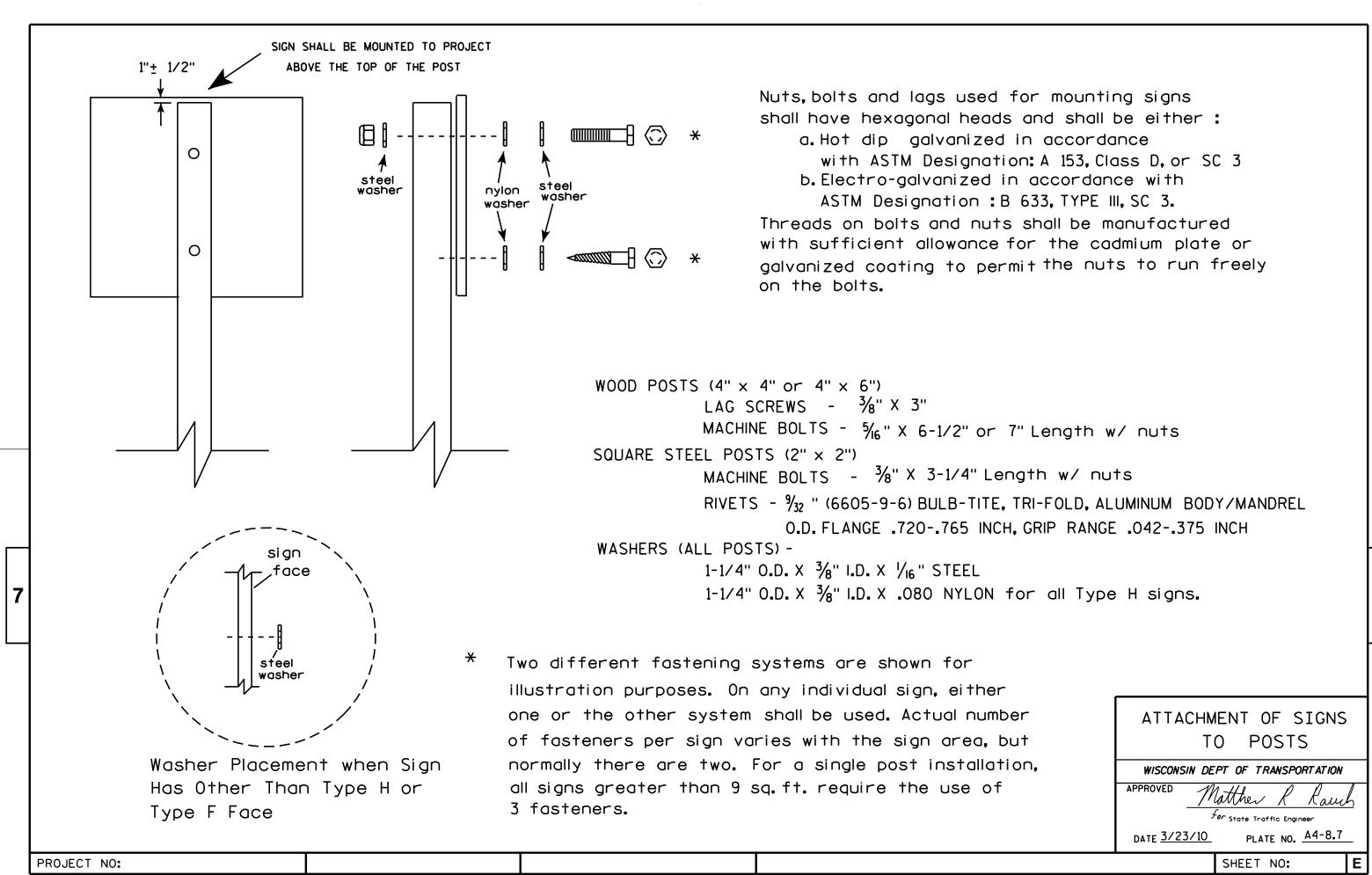
PROJECT NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A46.DGN

PLOT DATE: 05-DEC-2013 12:47

PLOT BY: mscs.ja







PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

For State Traffic Engineer

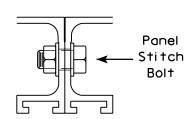


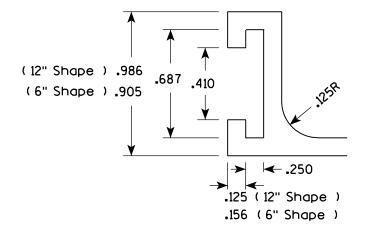
STITCH BOLT, WASHER & NUT

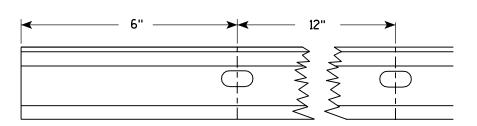
The hardware includes:

3/8 " - 16 X 3/4 " Economy Bolt 2024-T4 alloy 3/8 " - Stainless steel stop nut

3/8" X .064 Flat Washers, Alclad 2024-T4 alloy







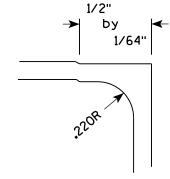
.078

← 2" →

6" Extrusion Minimum Weight 1.1 lb./ft.

←.125

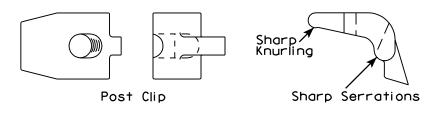
Punch $7/16" \times 7/8"$ oval holes beginning 6" in from end of extrusion 12" CC on both edges of 6" and 12" panels.

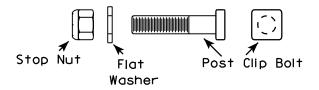


PLOT BY : DOTDZK

POST CLIP, POST CLIP BOLT, WASHER & NUT

Post Clip shall be Alum. Alloy 356-T6
Post Clip Bolt shall be Stainless Steel.
Flat washer shall be 3/8" X .091, Stainless Steel.
Stop nut shall be stainless steel.





NOTES

- 1. The contractor may select any brand of extrusion that conforms to the illustrations or meets with the approval of the engineer, but all extrusions used on this contract shall be of the same brand.
- 2. Panel Stitch Bolts shall be used to assemble adjacent panels. Maximum stitch bolt spacing shall be 24" C-C, and a minimum of 4 bolts shall be used to connect any two extrusions.
- 3. Post Clips shall be used to attach the sign panel to the sign support.

ALUMINUM EXTRUSIONS FOR TYPE I SIGNS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Les to J Spany

for State Traffic Engineer

DATE 11/18/99

PLATE NO. 45-2.9

SHEET NO:

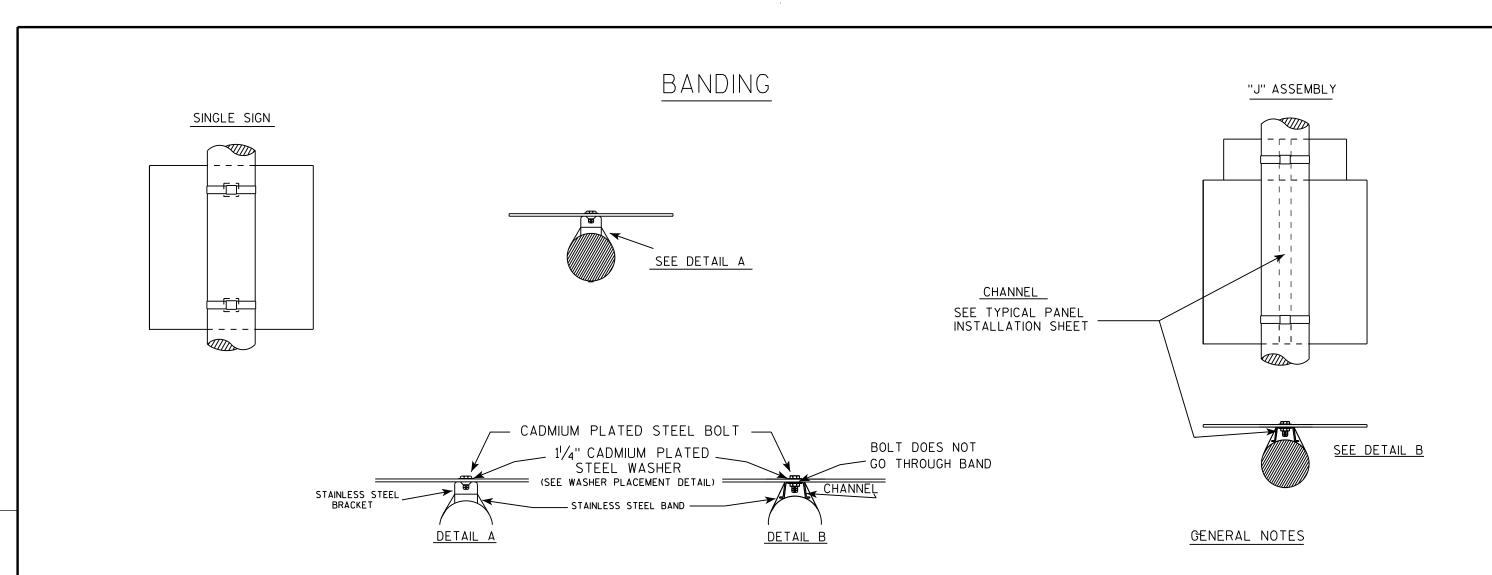
PROJECT NO:

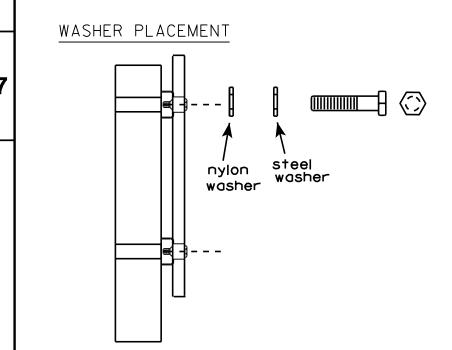
PLOT DATE: 28-SEP-2005 07:20

12" Extrusion

Minimum Weight

2.45 lb./ft.





HWY:

WASHERS (ALL POSTS) -

COUNTY:

1-1/4" O.D. $X\frac{3}{8}$ " I.D. $X\frac{1}{16}$ " STEEL 1-1/4" O.D. $X\frac{3}{8}$ " I.D. X .080 NYLON FOR ALL TYPE H SIGNS

PLOT BY: mscsja

- 1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
- 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
- 3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED 400 1100 400 400

For State Traffic Engineer

DATE 8/16/13

713 PLATE NO. A5-9.3

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A59.DGN

PROJECT NO:

PLOT DATE: 16-AUG-2013 13:27

PLOT NAME :

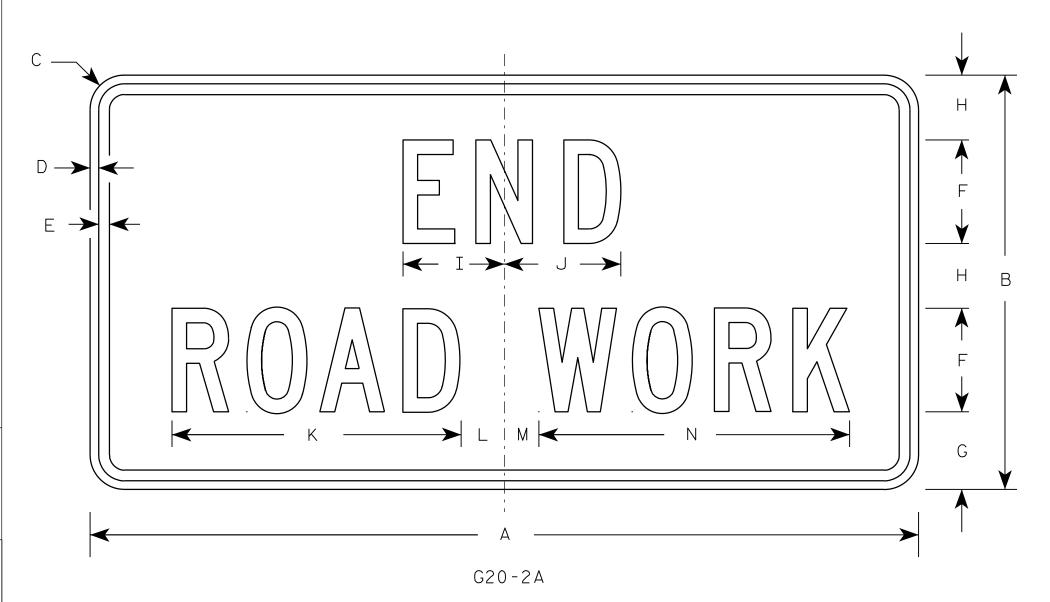
PLOT SCALE: 33.740899:1.000000

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

2. Color:

Background - Orange Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Metric equivalent for this sign is:

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	Q	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 %	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 1/8	6 3/4	16 ¾	2 1/2	1 3/4	18 ½													8.0	0.72

COUNTY:

STANDARD SIGN G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED 400 110 00 00 110

for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\G202A.DGN

HWY:

PROJECT NO:

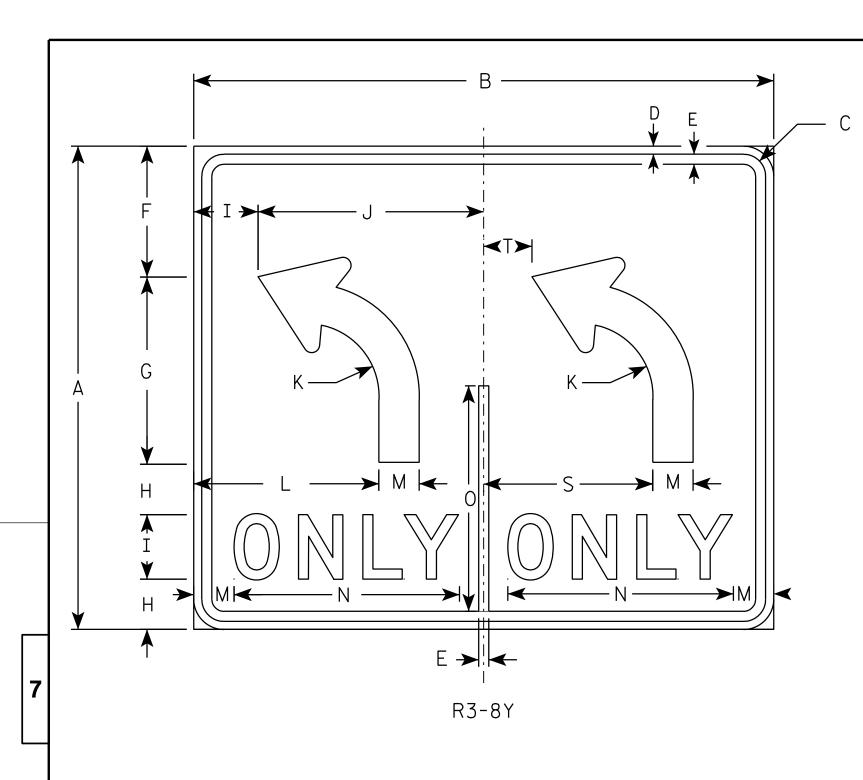
PLOT DATE: 30-SEP-2009 09:31

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE : 5.561773:1.000000

5.561773:1.000000 WISDOT/CADDS SHEET 42



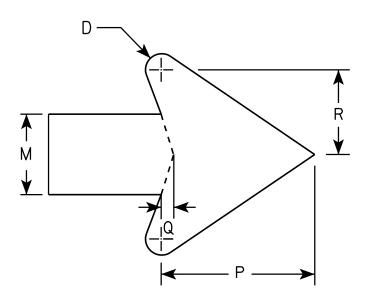
HWY:

NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - WHITE Message - BLACK

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	Х	Y	Z	Area sq. ft.
1																											
2S	30	36	1 3/8	1/2	5/8	8 1/8	11 5/8	3 1/8	4	14	4 1/2	11 1/2	2 1/2	14	14	4 3/4	3/8	2 %	10 1/2	3							7.5
2M	30	36	1 3/8	1/2	5/8	8 1/8	11 5/8	3 1/8	4	14	4 1/2	11 1/2	2 1/2	14	14	4 3/4	3/8	2 5/8	10 1/2	3							7.5
3																											
4	48	54	2 1/4	3/4	1	13 1/4	18 1/2	5 1/8	6	21	7 1/4	17 1/4	3 3/4	20 %	22 3/8	7 1/8	5/8	4	15 3/4	4 1/2							18.0
5	48	54	2 1/4	3/4	1	13 1/4	18 1/2	5 1/8	6	21	7 1/4	17 1/4	3 3/4	20 %	22 3/8	7 1/8	5/8	4	15 3/4	4 1/2							18.0
													_														

COUNTY:

STANDARD SIGN R3-8Y

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

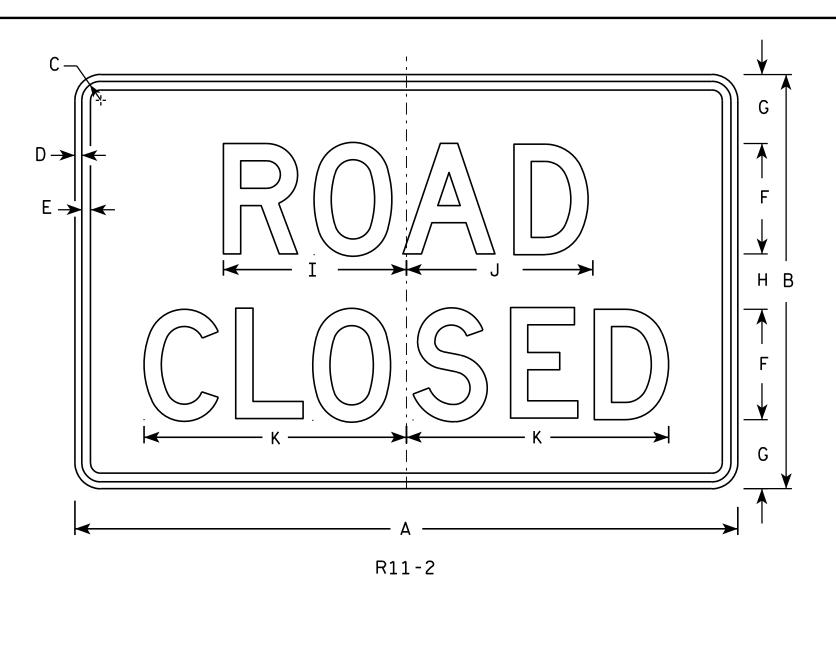
For State Traffic Engineer

DATE 3/24/2011 PLATE NO. R3-8Y.2

SHEET NO:

PROJECT NO:

PLOT BY: mscsja

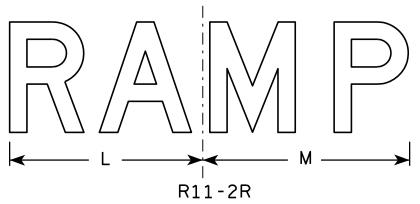


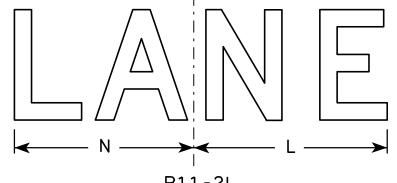
<u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Modify the message as required.





R	1	1	-	2	L

PLOT NAME :

SIZ	Έ	A	В	С	D	Ε	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																												
2	S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
21	I	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 ½	19	14	15	13													10.0
3		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
4		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
5		48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 ½	19	14	15	13													10.0

COUNTY:

STANDARD SIGN R11-2

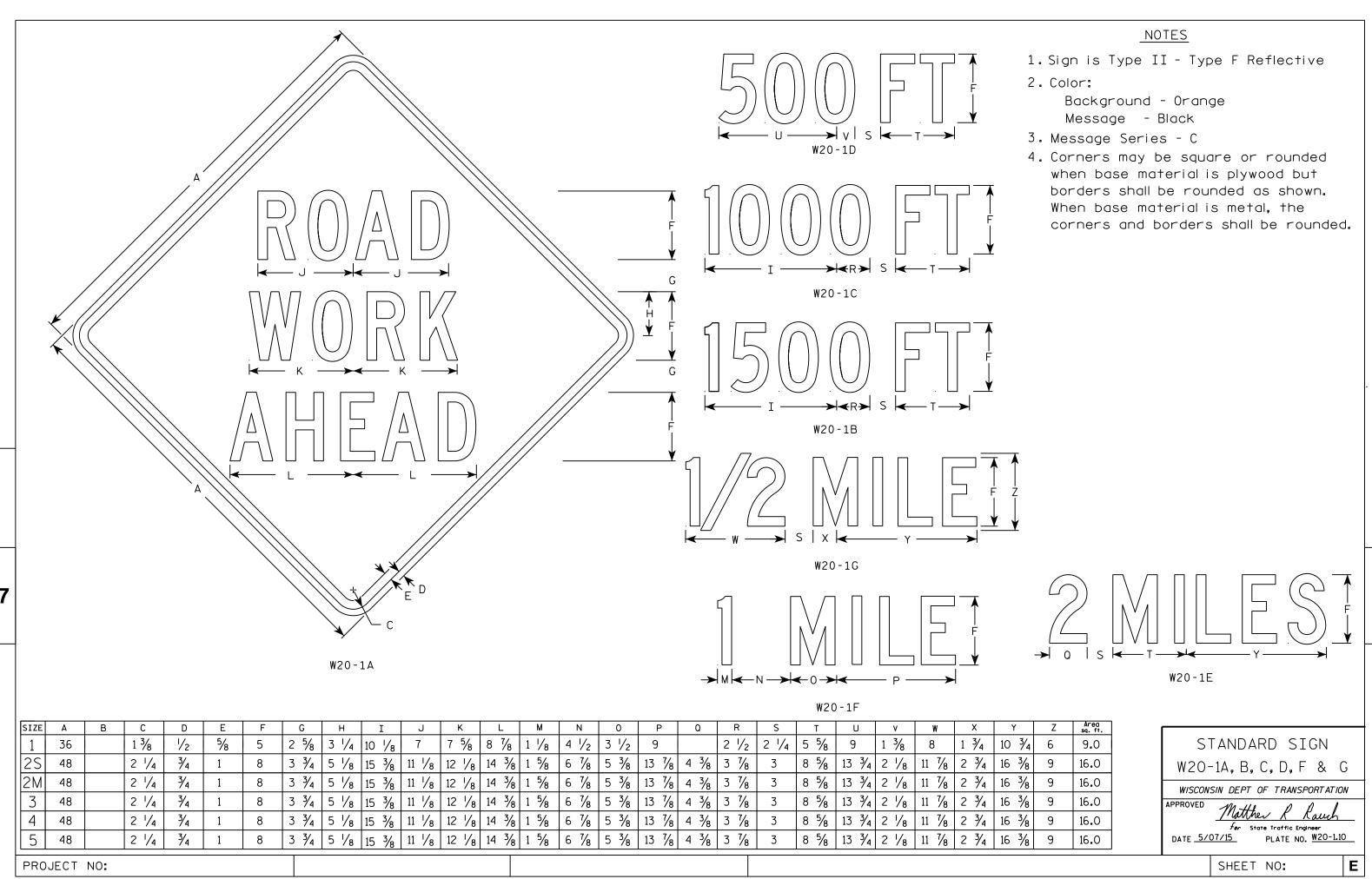
WISCONSIN DEPT OF TRANSPORTATION

DATE 4/1/11 PLATE NO. R11-2.10

SHEET NO:

HWY:

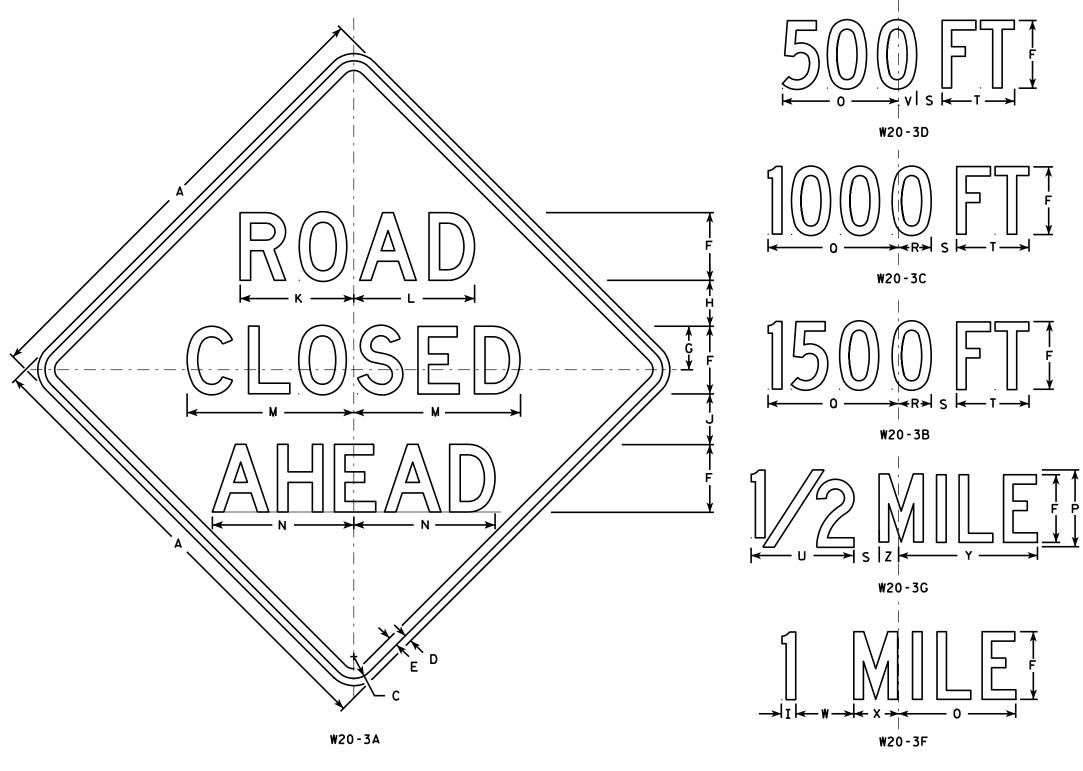
PROJECT NO:



FILE NAME . C.\CAFfiles\Projects\tr stdolote\W201 DCN

PLOT DATE . 01-DEC-2015 18.24

PIOT RY * \$\$ plotuser \$\$



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series see note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Lines 1 and 2 are Series D. Line 3 is Series D for AHEAD and Series C for all other distances.

1 % 5/8 ¾ 8 3/8 8 7/8 12 1/2 5 % 1 3/8 4 1/2 36 3 1/2 10 3/4 1 3/4 8 4 \(\frac{5}{8} \) 14 \(\frac{3}{8} \) 2 \(\frac{3}{8} \) 16.0 3/4 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 5/8 1 7/8 2M 3/4 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 48 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 7 1/2 10 % 1 % 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 3/4 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 4 % | 14 % | 2 % | 16.0 48 3/4 4 1/2 4 3/4 1 1/2 5 1/4 11 3/4 12 1/2 17 1/4 14 5/8 13 1/2 3 3/8 2 5/8 4 \\ 14 \\ 38 \ 2 \\ 38 \ 16.0 7 1/2 10 5/8 1 7/8 48 5 4 5/8 14 3/8 2 3/8 16.0 3/4 2 1/4 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 13 1/2 3 3/8 2 5/8 7 1/2 10 5/8 1 3/8 48

COUNTY:

STANDARD SIGN W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer DATE 3/18/11

PLATE NO. W20-3.7

SHEET NO:

PROJECT NO: FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN HWY:

PLOT DATE: 18-MAR-2011 12:08

PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: 9.931739:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. " _____ LANE" is Series B. All other copy is Series C.

W20-5D

W20-5B

W20-5G

PLOT BY: mscj9h

->IOI← R-		
	W20-5F	

								W20-	5 A																	11 2	20-56
SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	v	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	7/8	2 1/2	13 1/8	10 ¾	9 1/2	14 1/4	13 %	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 1/8	5 %	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 1/8	7 1/2	13 1/2	3 3/8	2 3/8	10 %	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 5/8	7 1/2	13 1/2	3 %	2 3/8	10 %	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 %	1 1/2	6	4 %	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 %	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 %	1 1/2	6	4 5/8	12	2 %	7 1/2	13 1/2	3 ¾	2 3/8	10 %	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 %	19	18 3/8	16	14 1/4	1 1/8	1 1/2	6	4 %	12	2 %	7 1/2	13 1/2	3 %	2 3/8	10 %	16.0

COUNTY:

STANDARD SIGN W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Kauch Fer State Traffic Engineer DATE 3/18/11 PLATE NO. W20-5.11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W205.DGN

PROJECT NO:

HWY:

W20-56A

W20-55A

PLOT DATE: 18-MAR-2011 12:15

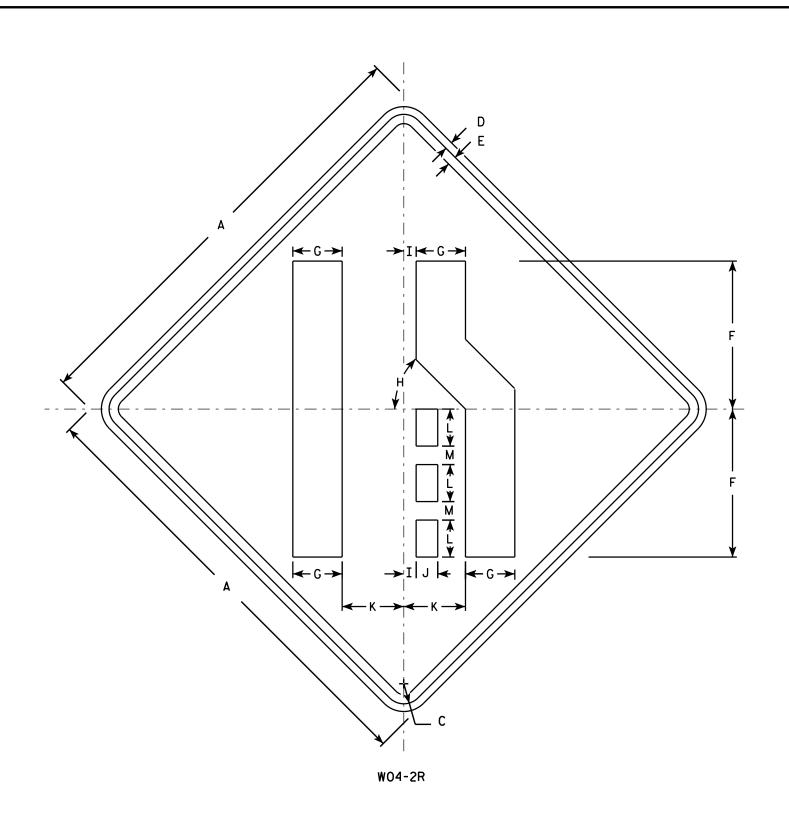
PLOT NAME :

PLOT SCALE: 11.918087:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W04-2L is the same as W04-2R except the symbolis reversed along the vertical centerline.



SIZE 1 % 5/8 3/4 12 45° 1 3/4 5 1 1/2 4 36 3 9.0 2S 2 1/4 5 3/8 45° 1 ¼ 2 ¾ 6 ¾ 3/4 48 16.0 45° 1 ¼ 2 ¾ 6 ¾ 3/4 5 3/8 48 2 1/4 2 16.0 2 1/4 3 48 3/4 5 % 45° | 1 1/4 | 2 3/8 | 6 3/4 2 16.0 2 1/4 3/4 5 3/8 45° | 1 1/4 | 2 3/8 | 6 3/4 48 2 16.0 5 2 1/4 3/4 5 3/8 45° | 1 1/4 | 2 3/8 | 6 3/4 48 2 16.0

STANDARD SIGN W04 - 2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

ForState Traffic Engineer

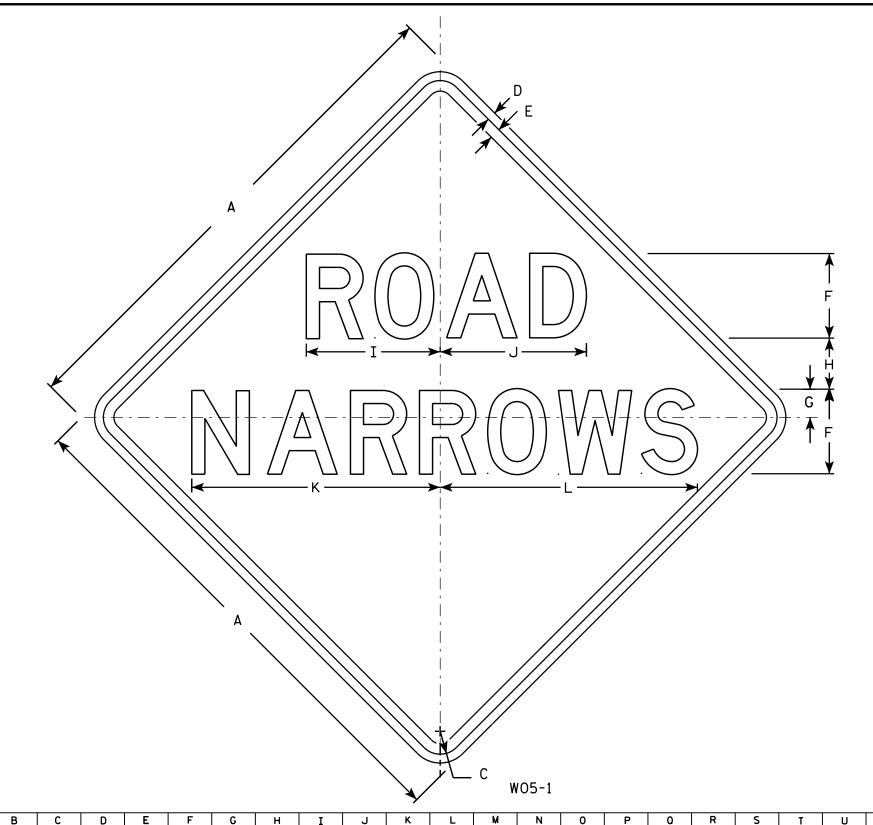
DATE 11/20/13 PLATE NO. <u>WO4-2.1</u>

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W042.DGN

PROJECT NO:

PLOT DATE: 20-NOV-2013 11:43



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE A 1 5/8 3 1/2 9 1/2 10 3/8 17 5/8 18 1/4 3/4 9.0 2S 2 1/4 3/4 12 3/4 13 3/4 23 1/2 24 3/8 48 16.0 2M 2 1/4 3/4 12 3/4 13 3/4 23 1/2 24 3/8 48 16.0 12 3/4 13 3/4 23 1/2 24 3/8 2 1/4 3/4 48 16.0 2 1/4 3/4 12 3/4 13 3/4 23 1/2 24 3/8 48 3 16.0 2 1/4 3/4 12 3/4 13 3/4 23 1/2 24 3/8 48 3 16.0

COUNTY:

STANDARD SIGN WO5-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

far State Traffic Engineer

DATE 11/20/13

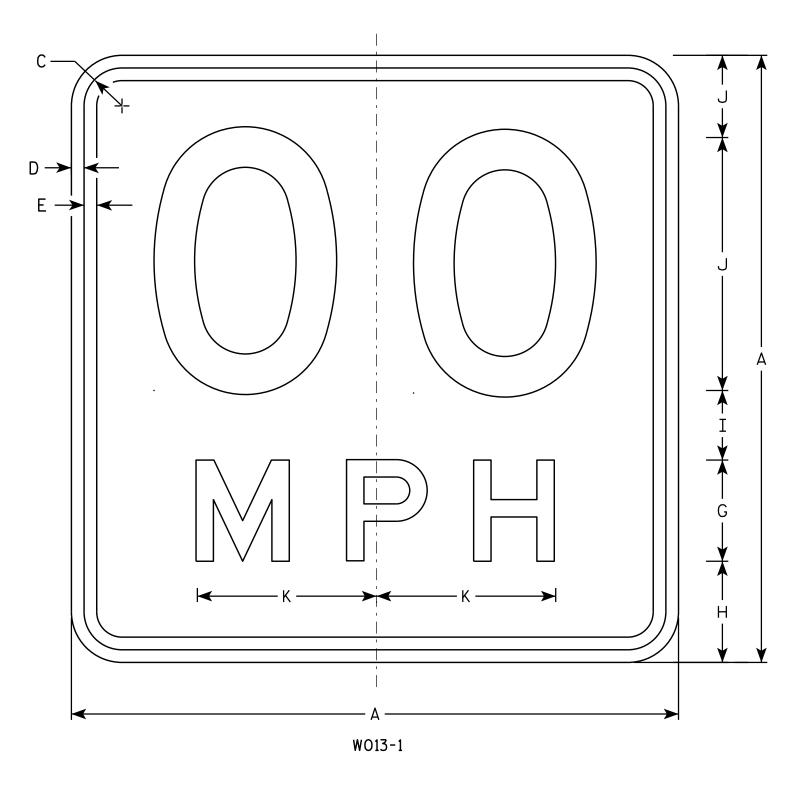
SHEET NO:

HWY:

PROJECT NO:

PLOT BY: mscsja

PLATE NO. W05-1.1



<u>NOTES</u>

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See Note 6
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
- 6. Line 1 is Series D Line 2 is Series E

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	T	U	٧	₩	Х	Y	Z	Areg sq. ft.
1	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	7 1/8																4.00
2S	36		1 5/8	5/8	₹4	16	6	5 1/2	4	4 1/2	10 %																9.00
2M	36		1 5/8	5/8	3∕4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
3	36		1 %	5/8	3/4	16	6	5 ½	4	4 1/2	10 %																9.00
4	36		1 %	5/8	3/4	16	6	5 1/2	4	4 1/2	10 %																9.00
5	36		1 %	5/8	3/4	16	6	5 1/2	4	4 1/2	10 %																9.00

COUNTY:

STANDARD SIGN W013-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R Rauch

For State Traffic Engineer

DATE 11/21/13 PLATE NO. WO13-1.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W0131.DGN

HWY:

PROJECT NO:

PLOT DATE: 02-DEC-2013 13:55

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 3.794391:1.000000

HIGHWAY

I-94

STH 165

STH 165 WB

STH 165

STH 165 WB

STH 165

STH 31 WEIGH STATION ENTRANCE RD

COUNTY ML COUNTY EZ

COUNTY EZ

I-894

LINCOLN MEMORIAL DR

LINCOLN MEMORIAL DR

STH 32 SB

STH 794 S USH 41 SB-STH 175 SB

> USH 41 STH 145 NB

GOOD HOPE RD

I-894

I-894

I-94

I-43

I-894

I-894

STH 38-NORTHWESTERN TH 38 NB-NORTHWEST

TH 38 SB-NORTHWEST

USH 12

COUNTY T

STH 164

COUNTY Y

STH 145 SB

STH 145 SB

I-94

COUNTY P

STH 83

STH 83

STH 83

1

1

1

ELEVATION VIEW SHEET NUMBER

9 OF 104

11 OF 104

12 OF 104

13 OF 104

14 OF 104

15 OF 104

16 OF 104

17 OF 104

18 OF 104

21 OF 104

26 OF 104

27 OF 104

30 OF 104

31 OF 104

33 OF 104

34 OF 104

37 OF 104

39 OF 104

41 OF 104

42 OF 104

43 OF 104

47 OF 104

50 OF 104

51 OF 104

52 OF 104

57 OF 104

64 OF 104

67 OF 104

79 OF 104

80 OF 104

81 OF 104

82 OF 104

84 OF 104

86 OF 104

88 OF 104

90 OF 104

91 OF 104

92 0F 104

94 OF 104

97 OF 104

100 OF 104

101 OF 104

102 OF 104

S-30-025

S-30-232

S-30-233

S-30-234

S-30-235

S-30-236

S-30-250

S-30-255

S-30-608

S-30-611

S-40-120

S-40-126

S-40-192

S-40-193 S-40-228

S-40-229

S-40-283

S-40-301

S-40-326

S-40-327

S-40-328

S-40-423

S-40-436

S-40-437

S-40-438

S-40-521

S-40-581

S-40-834

S-40-844

S-51-009

S-51-010

S-51-011

S-51-214

S-64-017

S-67-215

S-67-230

S-67-236

S-67-238

S-67-243

S-67-901

S-67-904

S-67-929

S-67-930

S-67-931

UNDISTRIBUTED

TOTAL

REMOVE GROUT

SPV.0060.02

4

SPV.0060.01

2

2

1

REPLACE RODENT TENSION ANCHOR RODS

SPV.0060.04

1

1 2

2

1

4

1

2

2

1

1

2

2

2

1

2

2

2

1

1

1

1

1 1

2

1

2

2

2

71

1

1

SPV.0060.05 FACH

SPV.0060.03

1000-20-68

LIST OF REPAIR DRAWINGS

1.	FOUNDATION DETAILS (1 OF 3)	52.	S-40-438
2.	FOUNDATION DETAILS (2 OF 3)	53.	S-40-443
3.	FOUNDATION DETAILS (3 OF 3)	54.	S-40-502
4.	TRUSS DETAILS (1 OF 2)	55.	S-40-503
5.	TRUSS DETAILS (2 OF 2)	56.	S-40-512
6.	SIGN PANEL DETAILS (1 OF 2)	57.	S-40-521
7.	SIGN PANEL DETAILS (2 OF 2)	58.	S-40-525
8.	ELECTRICAL DETAILS	59.	S-40-531
9.	S-30-025	60.	S-40-537
10.	S-30-035	61.	S-40-549
11.	S-30-232	62.	S-40-554
12.	S-30-233	63.	S-40-555
13.	S-30-234	64.	S-40-581
14.	S-30-235	65.	S-40-585
15.	S-30-236	66.	S-40-834
16.	S-30-250	67.	S-40-844
17.	S-30-255	68.	S-40-903
18.	S-30-608	69.	S-40-907
19.	S-30-609	70.	S-40-918
20.	S-30-610	71.	S-40-926
21.	S-30-611	72.	S-45-001
22.	S-30-612	73.	S-45-002
23.	S-40-009	74.	S-45-003
24.	S-40-018	75.	S-45-004
25.	S-40-049	76.	S-45-201
26.	S-40-120	77.	S-45-600
27.	S-40-126	78.	S-51-008
28.	S-40-151	79.	S-51-009
29.	S-40-185	80.	S-51-010
30.	S-40-192	81.	S-51-011
31.	S-40-193	82.	S-51-214
32.	S-40-204	83.	S-64-012
33.	S-40-228	84.	S-64-017
34.	S-40-229	85.	S-66-201
35.	S-40-268	86.	S-67-215
36.	S-40-269	87.	S-67-216
37.	S-40-283	88.	S-67-230
38.	S-40-285	89.	S-67-234
39.	S-40-301	90.	S-67-236
40.	S-40-306	91.	S-67-238
41.	S-40-326	92.	S-67-243
42.	S-40-327	93.	S-67-900
43.	S-40-328	94.	S-67-901
44.	S-40-352	95.	S-67-902
45.	S-40-355	96.	S-67-903
46.	S-40-393	97.	S-67-904
47.	S-40-423	98.	S-67-905
48.	S-40-425	99.	S-67-909
49.	S-40-426	100.	S-67-929
50.	S-40-436	101.	S-67-930
51.	S-40-437	102.	S-67-931
	- · · · · · · ·	103.	UNDISTRIBUTED QUANT
		100.	DETAILS (1 OF 2)

104. UNDISTRIBUTED QUANTITIES DETAILS (2 OF 2)



NO. DATE REVISION

COLLINS ENGINEERS 2

8



SIGN BRIDGE REPAIR AND REPLACEMENT 2016

SOUTHEAST REGION WIDE

COUNTY
SOUTHEAST REGION WIDE TOWN/CITY/VILLAGE VARIOUS

DESIGN SPEC.

AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS

DESIGNED VC CKD. PRT BY MR CKD.

FOUNDATION SHEET 1 OF 104 DETAILS (1 OF 3)

* THE INFORMATION	PROVIDED IN THIS TABLE IS	FOR INFORMATION	PURPOSES ONLY AND
	AS ADDITIONAL QUANTITIES	TO THOSE LISTED	ON THE ELEVATION VIEW
SHEETS.			

8

FILE NAME 108_02_found.dgn PLOT DATE :11/24/2015

PLOT BY: Veronica Chavezv8l DRV:Pdf_bw_HS_by level.plt PEN:CELTextSub.tbl

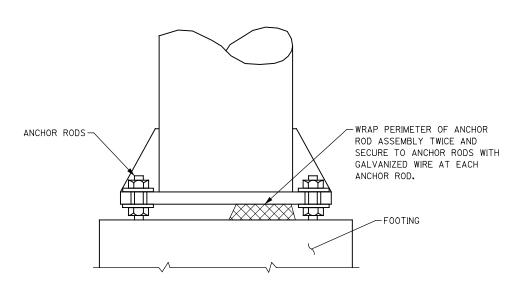
1000-20-68



1. CONCRETE - fc' = 3,500 P.S.I.

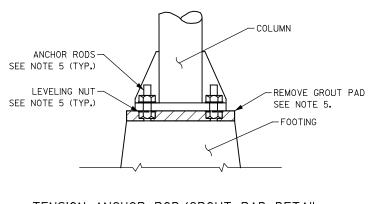
FOUNDATION NOTES:

- 2. BAR STEEL REINF. GRADE 60 fy = 60,000 P.S.I.
- 3. THE CONTRACTOR SHALL VERIFY EXACT LOCATION OF ALL REPAIRS.
- 4. THE CONTRACTOR SHALL FIELD VERIFY DIMENSION OF THE ITEM REQUIRED.
 DISCREPANCIES SHALL BE SUBMITTED TO THE ENGINEER FOR CLARIFICATION PRIOR
 TO BEGINNING WORK.
- 5. APPLY ZINC-RICH PAINT TO THE ANCHOR RODS, NUTS, WASHERS, AND LEVELING NUTS IN ACCORDANCE WITH SPECIFICATIONS AFTER REMOVING THE GROUT PAD OR TENSIONING THE ANCHOR ROD. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM, "REMOVE GROUT PAD" OR "TENSION ANCHOR RODS."
- 6. STEEL ANCHOR ROD NUTS AND WASHERS SHALL BE AASHTO M314 GRADE 55 Fy=55,000 PSI
- 7. THE TOP OF THE FOOTING SURFACE SHALL BE SMOOTHED AND SLOPED TO DRAIN.

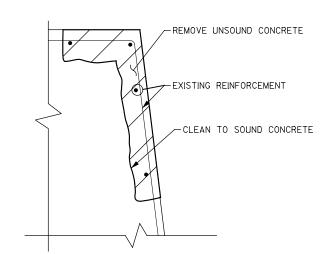


REPLACE RODENT SCREEN
REQUIRED ON ALL GROUT PAD REMOVALS

FOUNDATION DETAIL



TENSION ANCHOR ROD/GROUT PAD DETAIL



-COLUMN

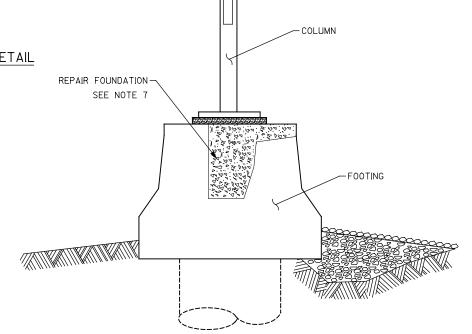
-REPAIR FOUNDATION

SEE NOTE 7

-FOOTING

-DRILLED SHAFT

REPAIR FOUNDATION DETAIL



FOUNDATION DETAIL

NO. DATE REVISION BY

STATE OF WISCONSIN

8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

9	SIGN	BRIDGE	REPAIR	AND	REF	PLACE	EMENT	2016
				DRAWN BY	М	R	PLANS CK'D.	RW
			_					

FOUNDATION SHEET 2 OF 104
DETAILS
(2 OF 3)

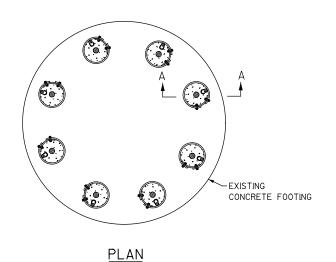
FILE NAME :08_02_found.dgn PLOT DATE :11/24/2015

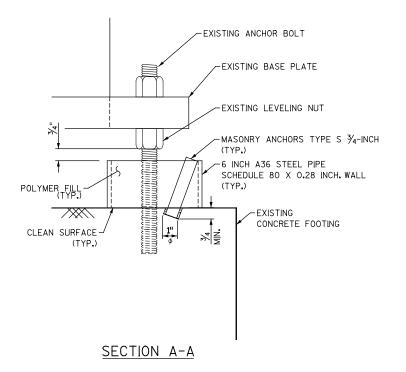
8

PLOT BY: Veronica Chavezy

Chavezv8l DRV:Pdf_bw_HS_by_level.plt PEN:CELTextSub.tbl

1000-20-68





FOUNDATION NOTES

DRAWINGS SHALL NOT BE SCALED

EXISTING GEOMETRY BASED OFF FIELD OBSERVATIONS AND INSPECTIONS, CONTRACTOR TO FIELD VERIFY PRIOR TO FABRICATION AND/OR ORDERING MATERIALS FOR ANCHOR BOLT

THE 6 INCH STEEL PIPE SHALL BE EITHER HOT-DIPPED GALVANIZED OR COLD GALVANIZED IN ACCORDANCE WITH THE A.A.S.H.T.O. SPECIFICATION AS STATED IN SECTION 641. OF THE WISDOT STANDARD SPECIFICATION.

THE ANCHOR BOLTS, NUTS, AND WASHERS SHALL BE COLD GALVANIZED WITH ZINC RICH PAINT IN ACCORDANCE TO THE

CLEAN ALL SURFACES AS SPECIFIED IN THE PLANS AND PER THE SPECIAL PROVISIONS. ALL CLEANING OF THE ITEMS ARE INCIDENTAL SIGN STRUCTURE ANCHOR ROD REPAIR.

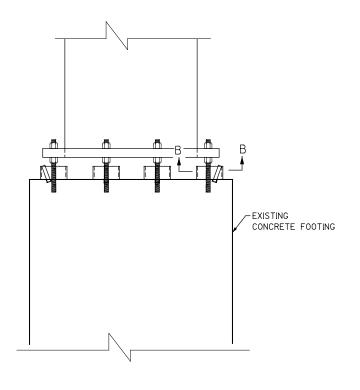
REFER TO THE SPECIAL PROVISIONS FOR POLYMER FILL

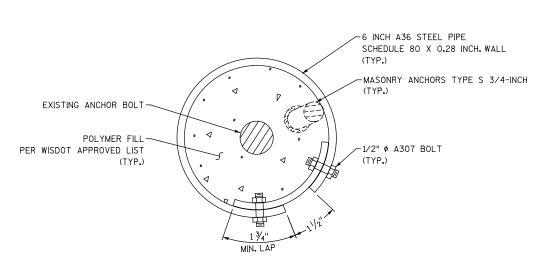
* THE NUMBER OF ANCHOR BOLTS SHOWN ARE FOR INFORMATION PURPOSES ONLY.

NUMBER OF ANCHOR BOLTS VARY PER STRUCTURE.

CONTRACTOR TO FIELD VERIFY NUMBER OF ANCHORS PER STRUCTURE NEEDED PRIOR TO ANY FABRICATION.

ALL ITEMS SHOWN ARE INCIDENTAL TO "SIGN STRUCTURE ANCHOR ROD REPAIR".





SECTION B-B

ELEVATION

SIGN STRUCTURE ANCHOR ROD REPAIR DETAIL

NO. DATE STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

SIGN BRIDGE REPAIR AND REPLACEMENT 2016

8

FOUNDATION SHEET 3 OF 104 DETAILS (3 OF 3)

FILE NAME : 08_02_found.dgn PLOT DATE : 11/24/2015

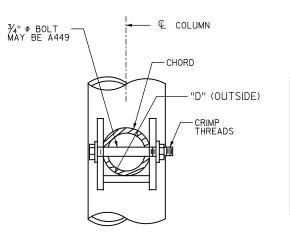
PLOT BY : Veronica Chavezv8i

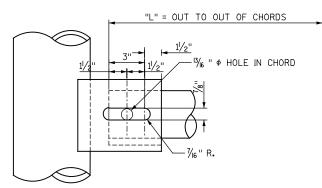
DRV :Pdf_bw_HS_by level.plt PEN :CEL_TextSub.tbl

TRUSS NOTES:

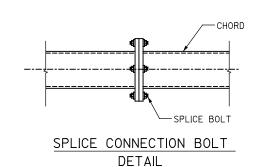
1000-20-68

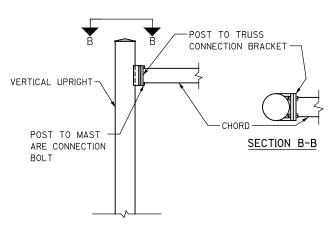
- 1. THE CONTRACTOR SHALL VERIFY EXACT LOCATION OF ALL REPAIRS.
- 2. THE CONTRACTOR SHALL FIELD VERIFY THE DIMENSIONS OF THE ITEM REQUIRED. DISCREPANCIES SHALL BE SUBMITTED TO THE ENGINEER FOR CLARIFICATION PRIOR TO BEGINNING WORK.
- 3. ALL STAINLESS STEEL BOLTS, LOCK WASHERS, AND NUTS SHALL CONFORM TO: -ANCHOR BOLTS/HEX BOLTS ASTM F593 ANY ALLOY GROUP 1, 2, OR 3 -HEX NUTS ASTM F594 -WASHERS ASTM A240 ANY OF THE 300 SERIES WHICH HAVE A MINIMUM YIELD OF 40,000 PSI AND ELONGATION OF 15% FOR OVER 3/4" \$\phi\$ AND 12% FOR 3/4" \$\phi\$ AND SMALLER.
- 4. REPLACE MISSING BOLTS ON TOWER CAPS WITH A STAINLESS STEEL BOLT.
- 5. REMOVE AND CLEAN ALL NESTING DEBRIS AT ALL POST TO CORD CONNECTIONS.





SADDLE POST TO TRUSS/CHORD DETAIL





OVERHEAD POST TO MAST ARM DETAIL

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION

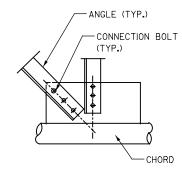
SIGN BRIDGE REPAIR AND REPLACEMENT 2016

TRUSS DETAILS (1 OF 2)

SHEET 4 OF 104

NO. DATE

8



TRUSS GUSSET CONNECTION DETAIL

VERTICAL SIGN SUPPORT

INSTALL U-BOL

SPV.0060.14

EACH

SPV.0060.15

EACH

INSTALL U-BOLT & TIGHTEN LOOSE U-BOLT DETAIL

SIGN SUPPORT BRACKET DETAIL

TABLE OF ESTIMATED QUANTITES FOR TRUSS*

SPV.0060.07

EACH

SPV.0060.08

EACH

SPV.0060.09

EACH

SPV.0060.10

EACH

SUPPORT BOLTS-

HORIZONTAL TRUSS MEMBER-

SUPPORT BRACKET

SPV.0060.11

EACH

SPV.0060.12

EACH

SPV.0060.13

EACH

HIGHWAY

STH 165

COUNTY MI

STH 145 SB

INCOLN MEMORIAL DR

HOWARD AVE

STH 145 WB

GOOD HOPE RD

USH 41

HIGHWAY

I-794

I-94

I-43

1-43

STH 57 EB

STH 38-NORTHWESTERN

H 38 SB-NORTHWE

USH 12

PARADISE DR EB

STH 83

STAINLESS STEEL U-BOLT WITH 2 LOCK WASHERS AND 2 HEX NUTS ATTACHED TO VERTICAL SIGN

-SIGN PANEL

VERTICAL SIGN SUPPORT

SUPPORT/L-BRACKET.

12

12

* THE INFORMATION PROVIDED IN THIS TABLE IS FOR INFORMATION PURPOSES ONLY AND NOT TO BE COUNTED AS ADDITIONAL QUANTITIES TO THOSE LISTED ON THE ELEVATION VIEW

ELEVATION VIEW SHEET NUMBER

15 OF 104

18 OF 104

29 OF 104

36 OF 104

38 OF 104

42 OF 104

48 OF 104

60 OF 104

62 OF 104

63 OF 104

65 OF 104 72 OF 104

73 OF 104

74 OF 104

75 OF 104

76 OF 104

78 OF 104

81 OF 104

83 OF 104

85 OF 104

88 OF 104

100 OF 104

S-30-035 S-30-236

S-30-608

S-40-185

S-40-268

S-40-269

S-40-285

S-40-306

S-40-327

S-40-425

S-40-537

S-40-554

S-40-555

S-40-585

S-45-002

S-45-003

S-45-004

S-45-201

S-51-008

S-51-011 S-64-012

S-64-017

S-66-201

S-67-909

S-67-929

UNDISTRIBUTED

TOTAL

SHEETS.

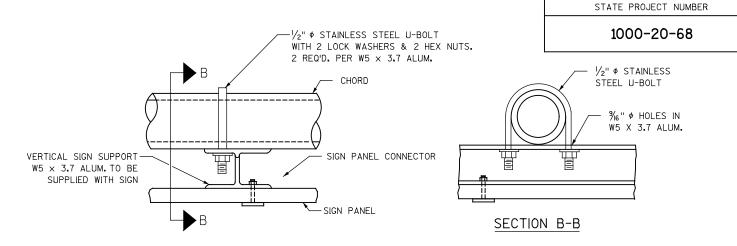
STATE PROJECT NUMBER 1000-20-68 TRUSS NOTES: - COLUMN 1. WHEN A FULL SPAN SIGN BRIDGE IS OVER BOTH DIRECTIONS AND SIGNS ARE ON STRUCTURE FOR BOTH DIRECTIONS, THEN NORTHBOUND AND EASTBOUND GOVERN THE NUMBERING SYSTEM. 2. THE CANTILEVER SIGN BRIDGE NUMBERING SYSTEM ALWAYS COUNTS UP FROM LEFT TO RIGHT REGARDLESS OF COLUMN LOCATION. - CHORD (TYP.) 3. TYPICAL SIGN BRIDGE CONFIGURATION FOR INFORMATION ONLY. TRAFFIC CANTILEVER SIGN BRIDGE -COLUMN (TYP.) WEB (TYP.) CHORD (TYP.) - COUPLING (TYP.) TRAFFIC 4-CHORD FULL SPAN SIGN BRIGE UPPER CHORD-UF4 SPLICE COUPLING (TYP.)-DIAGONAL -MIDDLE BACK-CHORD MB2 DIAGONAL PERPENDICULARS (EACH END OF UNITS ONLY) TRAFFIC NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION LF1 -LOWER CHORD 3-CHORD FULL SPAN SIGN BRIGE SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SHEET 5 OF 104 TRUSS DETAILS (2 OF 2)

8

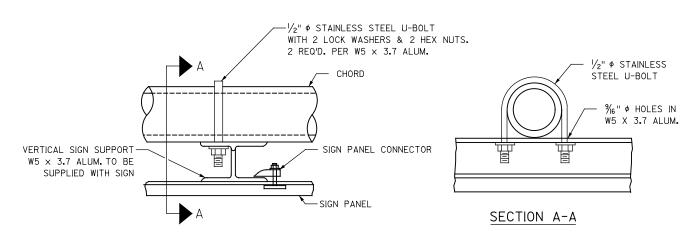
TABLE OF ESTIMATED QUANTITES FOR SIGNS*

STRUCTURE NUMBER	ELEVATION VIEW	HIGHWAY	REPLACE SIGN BRIDGE ID PLAQUE	REPLACE SIGN PANEL CONNECTOR	REATTACH TYPE-II SIGN	REPLACE TYPE-II SIGN	INSTALL OVERHEAD SIGN MOUNTING SYSTEM	REPLACE SIGN CONNECTION ASSEMBLY	SLOTTED VERTICAL SIGN SUPPORT REPAIR	INSTALL VERITO SIGN SUPPOR
HOMBER	SHEET NUMBER		SPV.0060.16	SPV.0060.17	SPV.0060.18	SPV.0165.01	SPV.0060.19	SPV.0060.20	SPV.0060.21	SPV.0060.22
			EACH	EACH	EACH	SF	EACH	EACH	EACH	EACH
S-30-035	10 OF 104	I-94		2						
S-30-232	11 OF 104	STH 165							4	
S-30-233	12 OF 104	STH 165 WB							4	
S-30-234	13 OF 104	STH 165							5	
S-30-235	14 OF 104	STH 165 WB							4	
S-30-255	17 OF 104	WEIGH STATION ENTRANCE RD	1							
S-30-608	18 OF 104	COUNTY ML							4	
S-30-609	19 OF 104	COUNTY ML							4	
S-30-610	20 OF 104	COUNTY EZ							4	
S-30-611	21 OF 104	COUNTY EZ							4	
S-30-612	22 OF 104	WEST FRONTAGE RD	1							
S-40-009	23 OF 104	J-894		16						
S-40-018	24 OF 104	I-894								7
S-40-204	32 OF 104	I-94			1					
S-40-301	39 OF 104	STH 794 S	1							
S-40-327	42 OF 104	USH 41		40						
S-40-438	52 OF 104	I-94	1							
S-40-531	59 OF 104	794				20				
S-40-554	62 OF 104	I-94		2						
S-40-844	67 OF 104	STH 145		1						
S-40-918	70 OF 104	STH 181	1							
S-40-926	71 OF 104	COUNTY PP E							8	
S-45-201	76 OF 104	STH 57 EB	1							
S-45-600	77 OF 104	PORT WASHINGTON RD	1							
S-51-010	80 OF 104	STH 38 SB-NORTHWEST AVE			1					
S-66-201	85 OF 104	PARADISE DR EB						1		
S-67-215	86 OF 104	COUNTY T					1			
S-67-216	87 OF 104	COUNTY T					1			
S-67-900	93 OF 104	I-94							5	
S-67-901	94 OF 104	I-94							6	
S-67-902	95 OF 104	COUNTY P							6	
S-67-903	96 OF 104	COUNTY P							6	
S-67-904	97 OF 104	COUNTY P							6	
S-67-905	98 OF 104	PABST FARD RD							4	
S-67-929	100 OF 104	STH 83	1						4	
S-67-930	101 OF 104	STH 83	1						4	
S-67-931	102 OF 104	STH 83	1						5	
UNDISTRIBUTED		1								
TOTAL			10	61	2	20	2	1	87	7

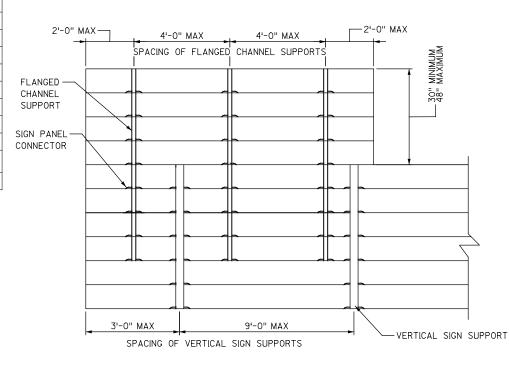
^{*} THE INFORMATION PROVIDED IN THIS TABLE IS FOR INFORMATION PURPOSES ONLY AND NOT TO BE COUNTED AS ADDITIONAL QUANTITIES TO THOSE LISTED ON THE ELEVATION VIEW SHEETS.



TYPICAL SIGN PANEL CONNECTION (TYPE II SIGN)



TYPICAL SIGN PANEL CONNECTION (TYPE ISIGN)



FLANGED CHANNEL SUPPORT/VERTICAL SIGN SUPPORT DETAILS

DEPARTMENT OF WISCONSIN
STRUCTURES DESIGN SECTION

SIGN BRIDGE REPAIR AND REPLACEMENT 2016

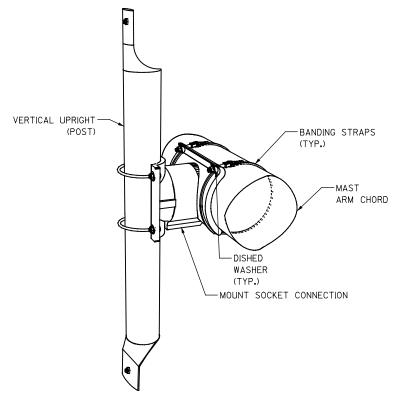
DRAWN MR PLANS
PW

8

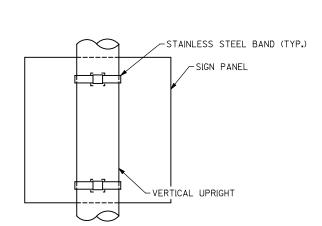
DRAWN MR PLANS RW
SIGN PANEL
DETAILS
(1 OF 2)

8

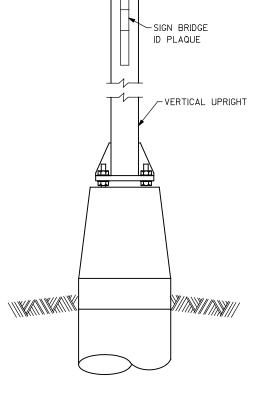
1000-20-68



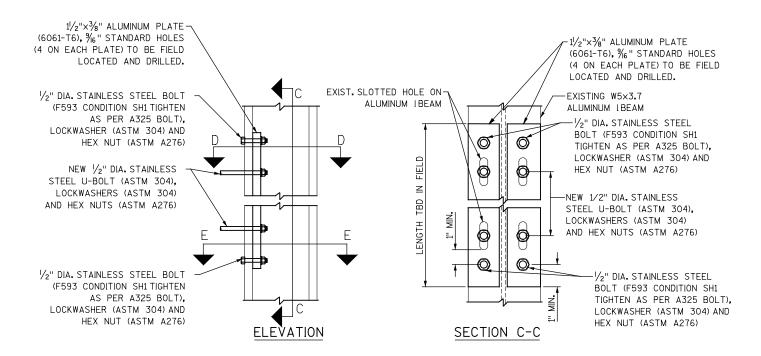
POST TO CHORD CONNECTION DETAIL (TYPE II SIGN) /SIGN CONNECTION ASSEMBLY DETAIL

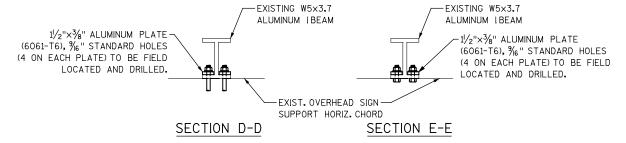


REATTACH TYPE-II SIGN TO VERTICAL UPRIGHT REFER TO SIGN PLATE MANUAL FOR DETAILS (SIGN BANDING DETAILS)



SIGN BRIDGE ID PLAQUE





SLOTTED VERTICAL SIGN SUPPORT REPAIR DETAIL

EACH REPAIR INCLUDES ALL BOLT CONNECTIONS PER I-BEAM

				Γ
NO.	DATE	REVISION	BY	
		STATE OF WISCONISIN		ı

8

DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION

SIGN BRIDGE REPAIR	AND	REP	LACE	EME	ENT	2016	
	DRAWN BY	MF	~	PL/ CK'l	ANS D.	RW]
SIGN PAN	=		SHEE	т.	7	0F 104	1

DETAILS (2 OF 2)

FILE NAME :08_05_stgn.dgn PLOT DATE :11/24/2015

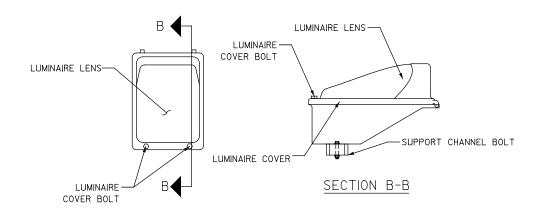
8

DRV :Pdf_bw_HS_by level.plt PEN :CEL_TextSub.tbl

1000-20-68

ELECTRICAL NOTES:

- 1. THE CONTRACTOR SHALL VERIFY EXACT LOCATION OF ALL REPAIRS.
- 2. THE CONTRACTOR SHALL FIELD VERIFY THE SIZE OF THE ITEM REQUIRED.
- 3. ALL STAINLESS STEEL BOLTS, LOCK WASHERS, AND NUTS SHALL CONFORM TO:
 - -ANCHOR BOLTS/HEX BOLTS ASTM F593 ANY ALLOY GROUP 1, 2, OR 3
- -HEX NUTS ASTM F594 -WASHERS ASTM A240
- ANY OF THE 300 SERIES WHICH HAVE A MINIMUM YIELD OF 40,000 PSI AND ELONGATION OF 15% FOR OVER $\frac{3}{4}$ " ϕ AND 12% FOR $\frac{3}{4}$ " ϕ AND SMALLER.
- 4. THE CONTRACTOR SHALL USE ANTI-SIEZE COMPOUND ON ELECTRICAL HANDHOLE COVER BOLTS, JUNCTION BOX BOLTS, LUMINAIRE COVER BOLTS, AND CONDUIT PLUGS PER SECTION 651.3.1 (5) OF THE WISDOT STANDARD SPECIFICATION.
- 5. CAP ALL EXPOSED WIRES AND CLOSE THE JUNCTION BOX.
- 6. REPLACE THE JUNCTION BOX BOLT WITH A STAINLESS STEEL BOLT.
- 7. AISM TRAFFIC COUNTERS TO BE TURNED OVER TO TAPCO, BRIAN SCHARLES: 262-814-7306



TYPICAL LUMINAIRE DETAILS

NO.	DATE		RE	EVISION	1			BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION								
CI	מם ואר	IDCE E		V VID	DEE	N ACEN	IENIT	2016

ECTRICAL SHEET 8 OF 104

8

ELECTRICAL DETAILS

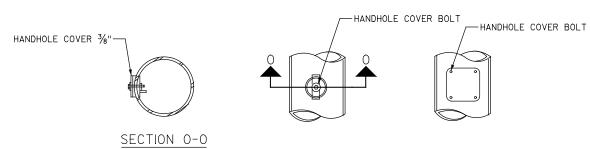
TABLE OF ESTIMATED QUANTITES FOR ELECTRICAL*

TIGHTEN SIGNAL MOUNT SOCKET CONNECTION REMOVE ASIM SECURE HANDHOLE SECURE LUMINAIRE INSTALL JUNCTION COVER BOX COVER REPLACE CONDUIT TRAFFIC COUNTER UNITS STRUCTURE NUMBER ELEVATION VIEW SHEET NUMBER HIGHWAY SPV.0060.23 SPV.0060.24 SPV.0060.25 SPV.0060.26 SPV.0060.27 SPV.0060.28 SPV.0060.29 EACH 24 OF 104 S-40-018 l-894 2 S-40-328 43 OF 104 STH 145 NB S-40-352 44 OF 104 USH 45 1 S-40-355 45 OF 104 USH 45 46 OF 104 S-40-393 1-43 S-40-423 47 OF 104 GOOD HOPE RD S-40-443 53 OF 104 I-94 S-40-502 54 OF 104 1-43 6 55 OF 104 S-40-503 I-43 S-40-512 56 OF 104 1-43 4 S-40-521 57 OF 104 I-43 S-40-525 58 OF 104 I-94 S-40-549 61 OF 104 67 OF 104 STH 145 S-40-844 S-40-903 68 OF 104 STH 175 NB UNDISTRIBUTED 2 2 26

FILE NAME :08_09_elect.dgn PLOT DATE :11/24/2015

PLOT BY : Veronica Chavezv8i

DRV :Pdf_bw_HS_by level.plt PEN :CEL_TextSub.tbl

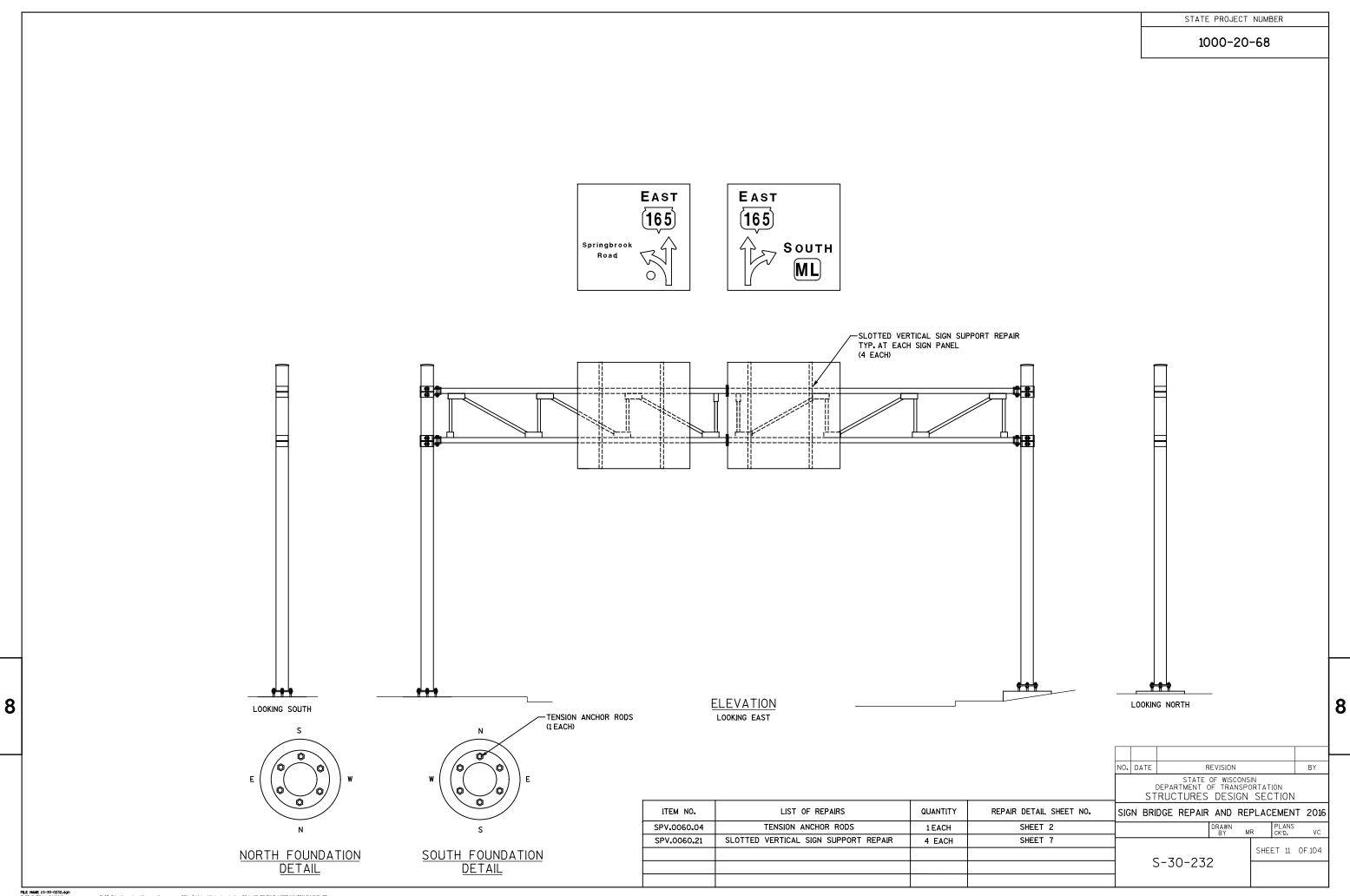


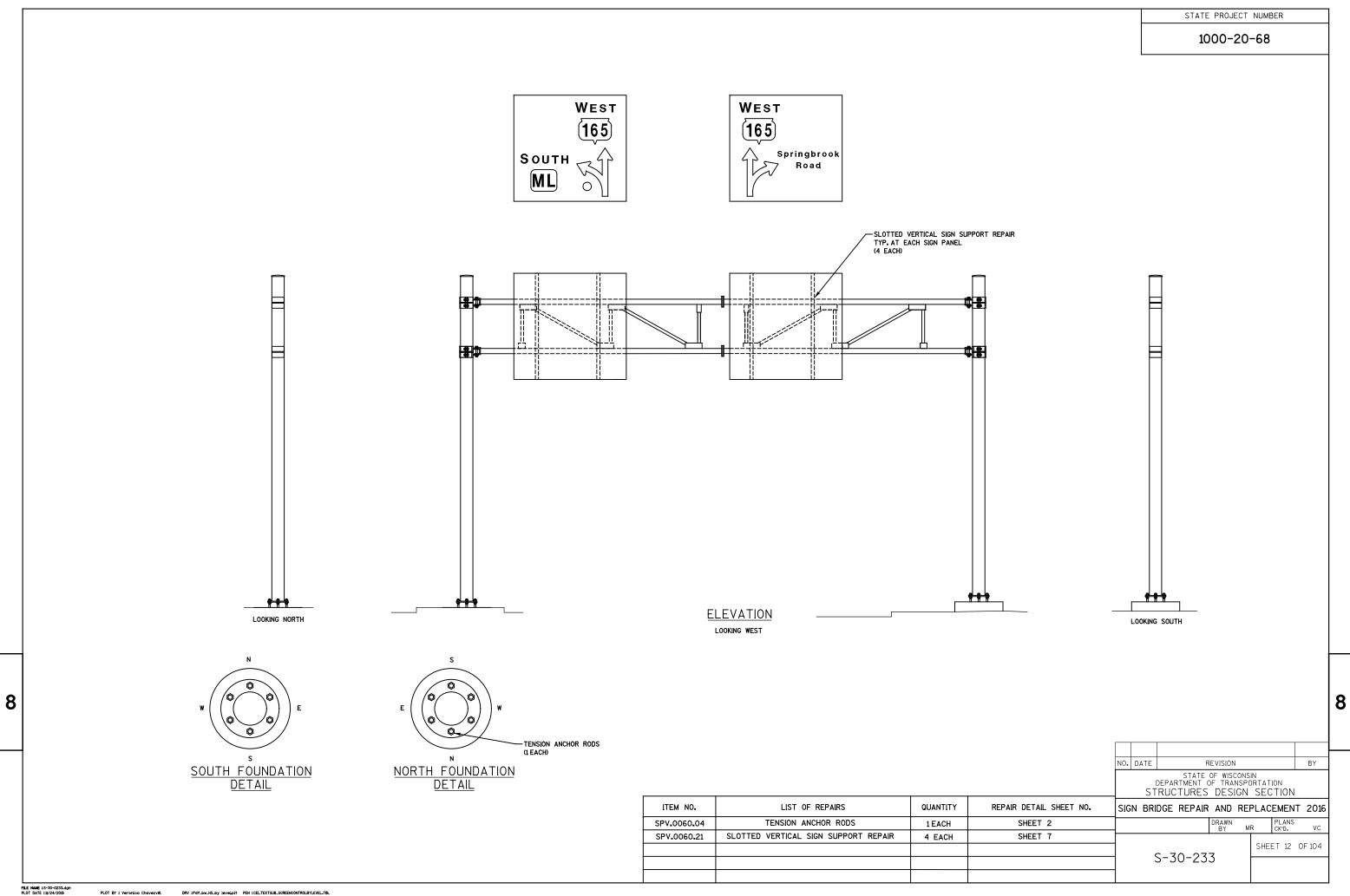
HANDHOLE DETAILS

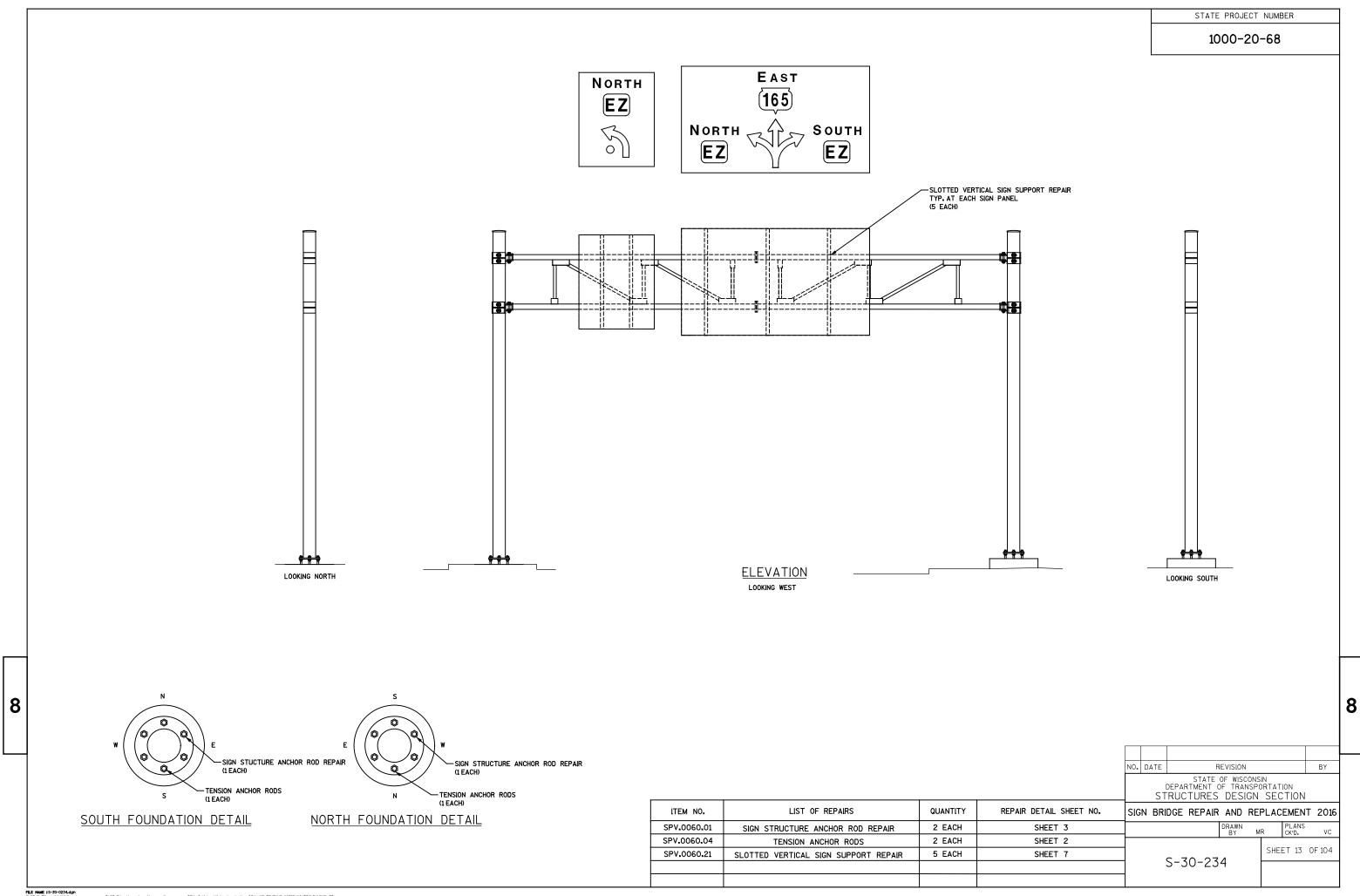
 $[\]star$ the information provided in this table is for information purposes only and not to be counted as additional quantities to those listed on the elevation view sheets.

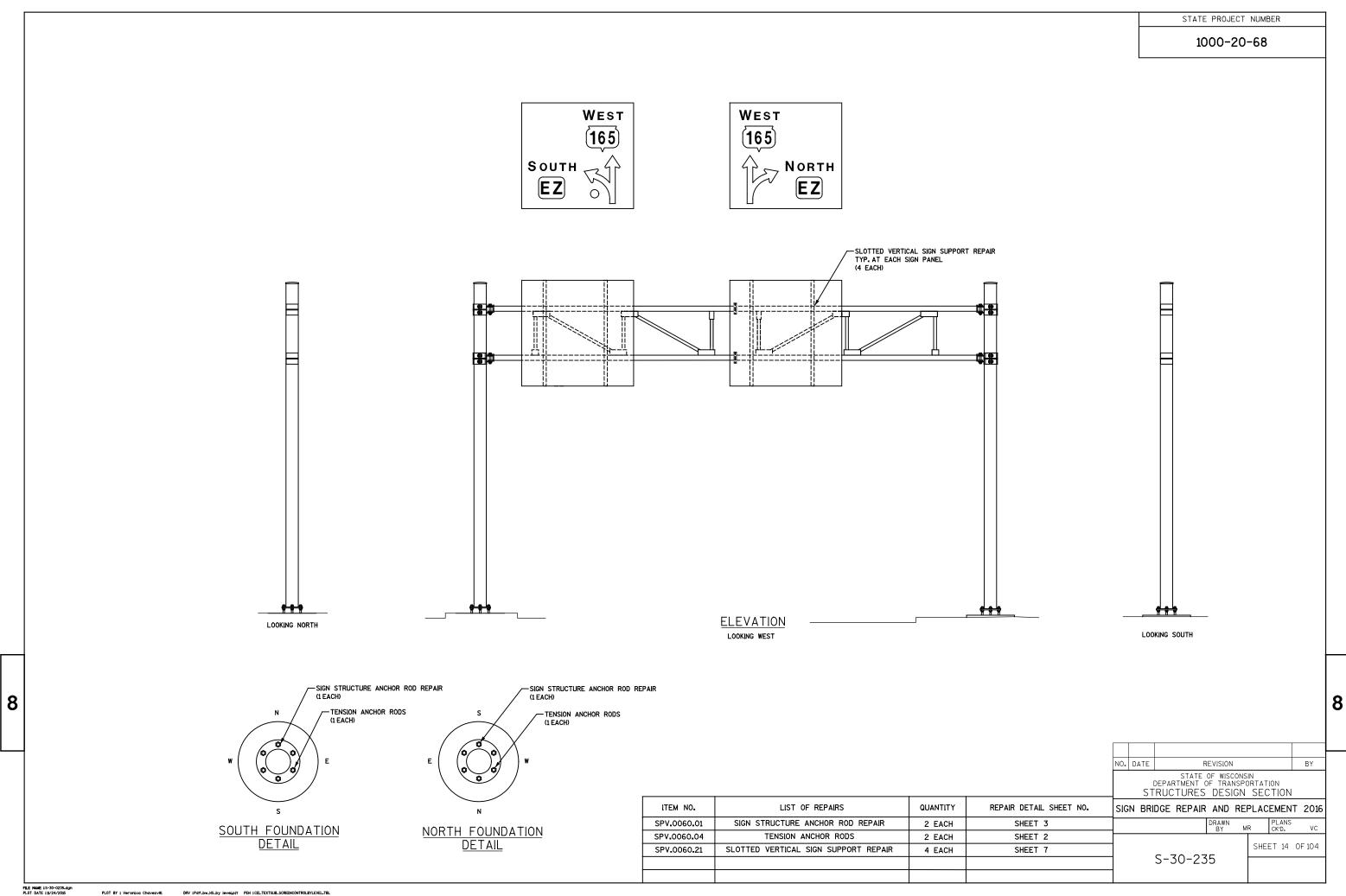
STATE PROJECT NUMBER 1000-20-68 Weigh Station EXIT ONLY -TENSION ANCHOR RODS (1 EACH) 8 **ELEVATION** LOOKING NORTH LOOKING WEST NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 1 EACH SHEET 2 FOUNDATION DETAIL SHEET 9 OF 104 S-30-025 FILE NAME : S-30-0025.dgn PLOT DATE : 11/24/2015 PLOT BY: Veronica Chavezv8i DRV: Pdf_bw_HS_by level.plt PEN: CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL

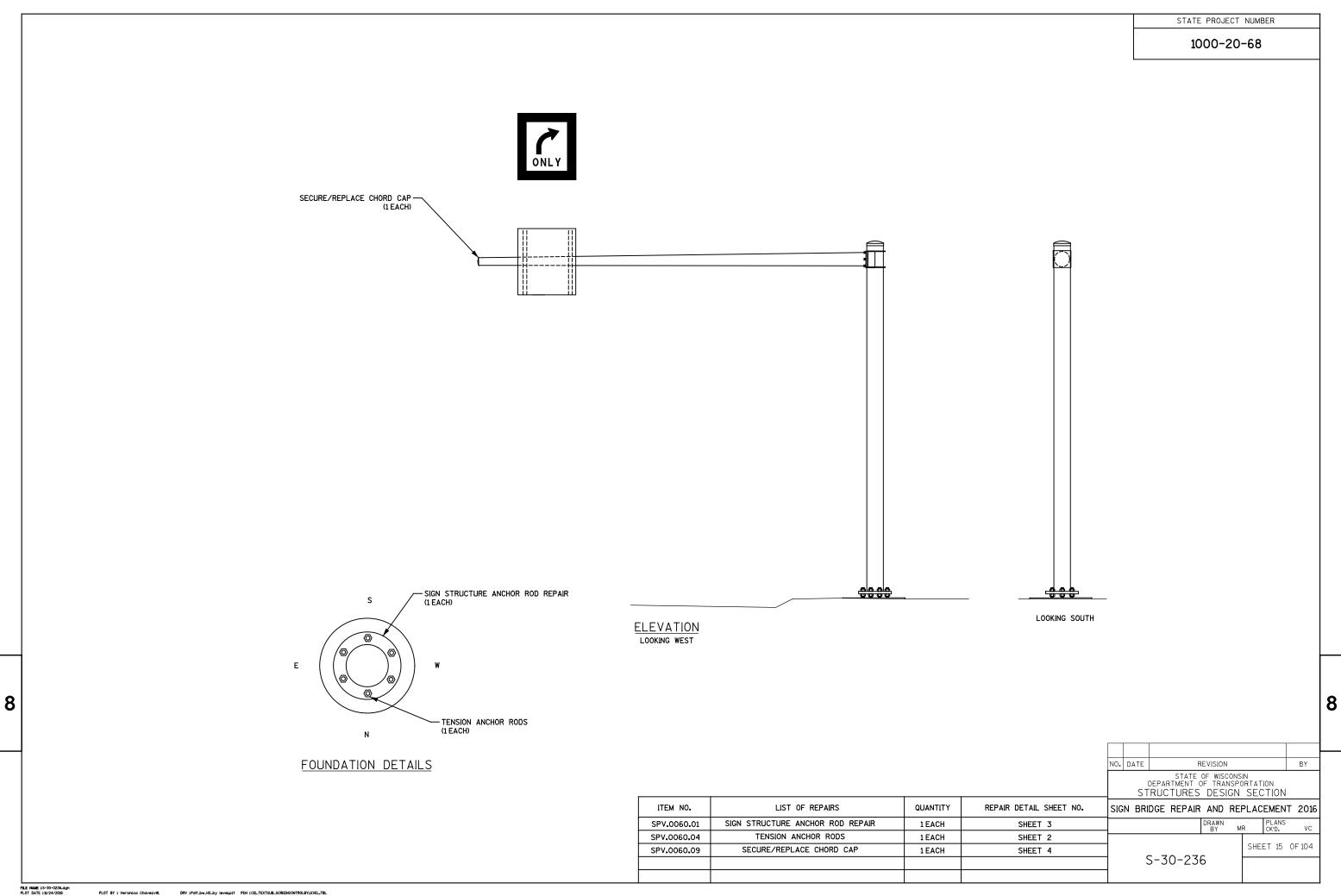
STATE PROJECT NUMBER 1000-20-68 EXIT 1A A1 county Russell Rd 1/4 MILE SPLICE CONNECTION BOLT REPLACEMENT (2 EACH) -REPLACE SIGN PANEL CONNECTOR (2 EACH) ELEVATION LOOKING SOUTH LOOKING WEST LOOKING EAST 8 NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 EAST FOUNDATION DETAIL SPV.0060.07 SPLICE CONNECTION BOLT REPLACEMENT 2 EACH SHEET 4 WEST FOUNDATION DETAIL SPV.0060.17 2 EACH SHEET 6 REPLACE SIGN PANEL CONNECTOR SHEET 10 OF 104 S-30-035 FILE NAME : S-30-0035,dgn PLOT DATE : 11/24/2015 DRV :Pdf_bw_HS_by level.pit PEN :CEL_TEXTSUB_SCREENCONTROLBYLEVEL_TBL

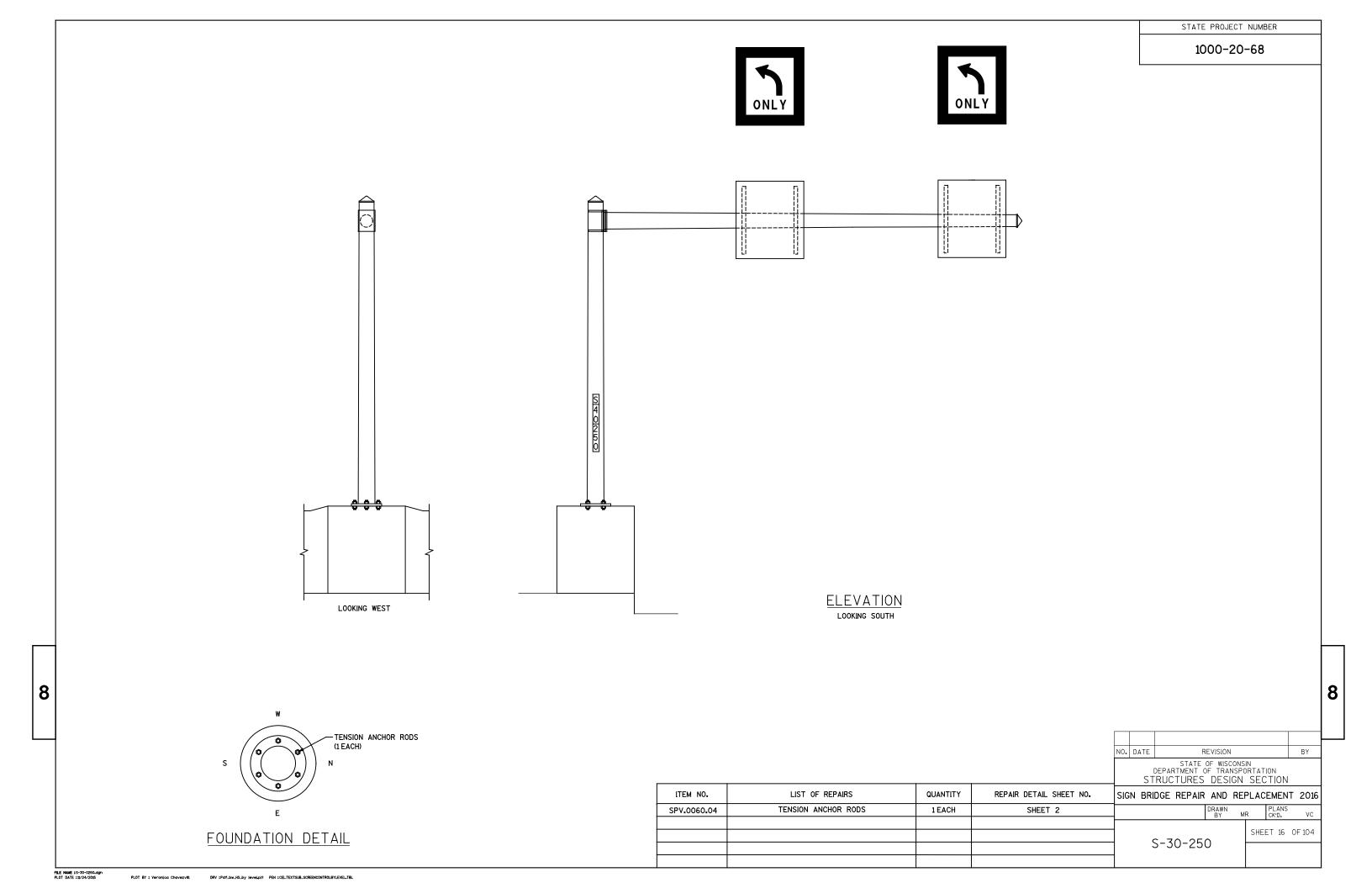




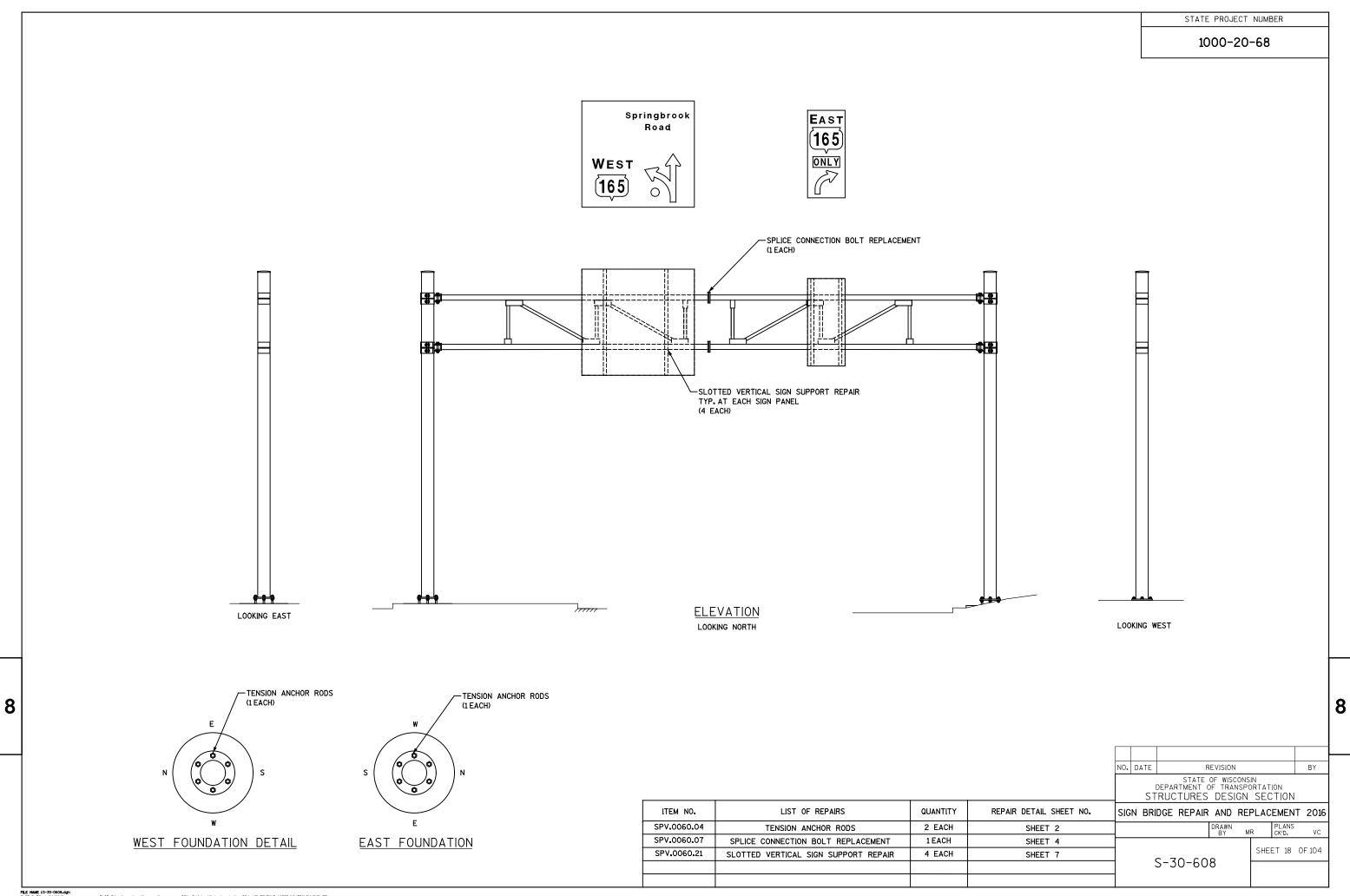


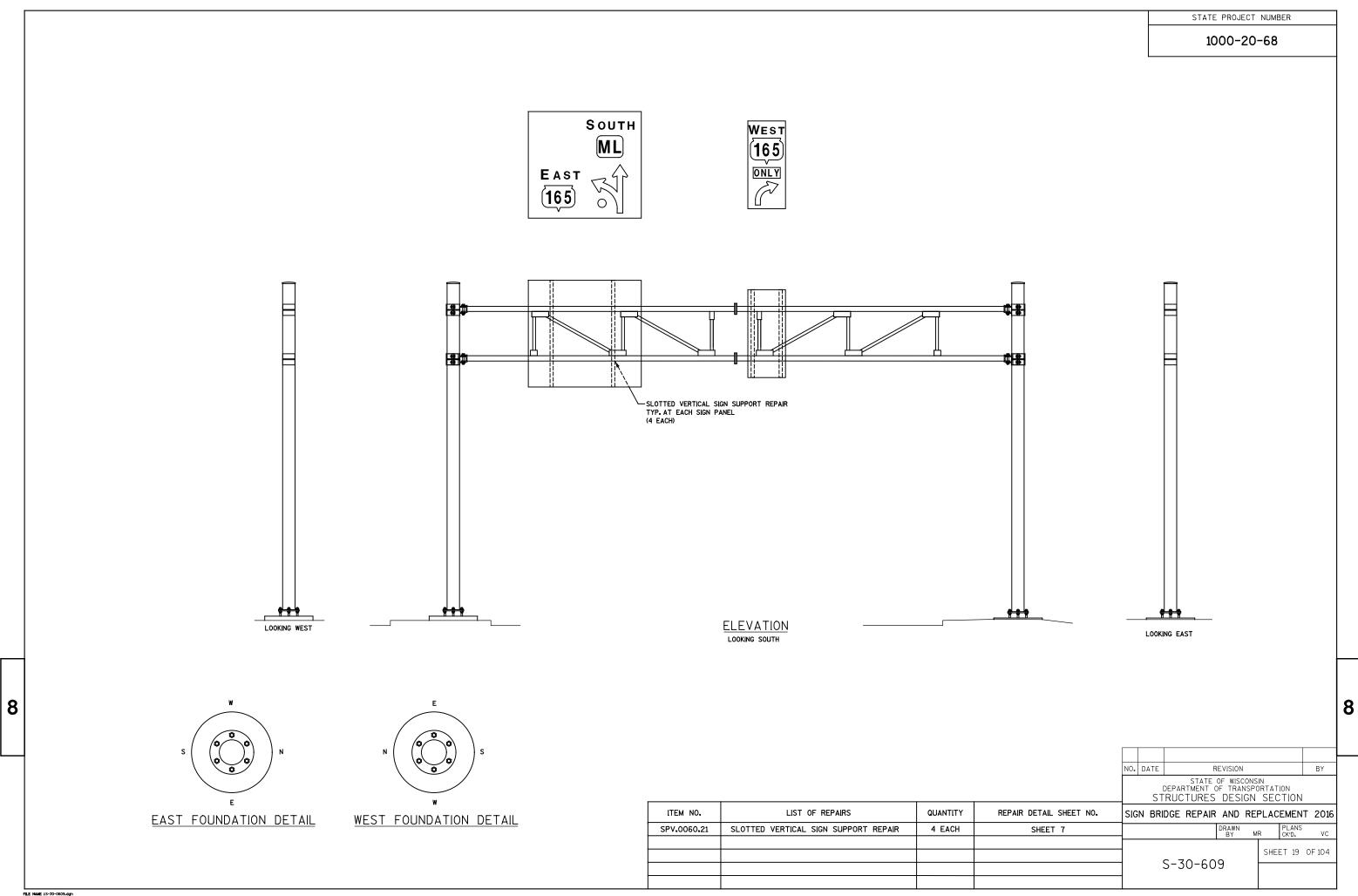


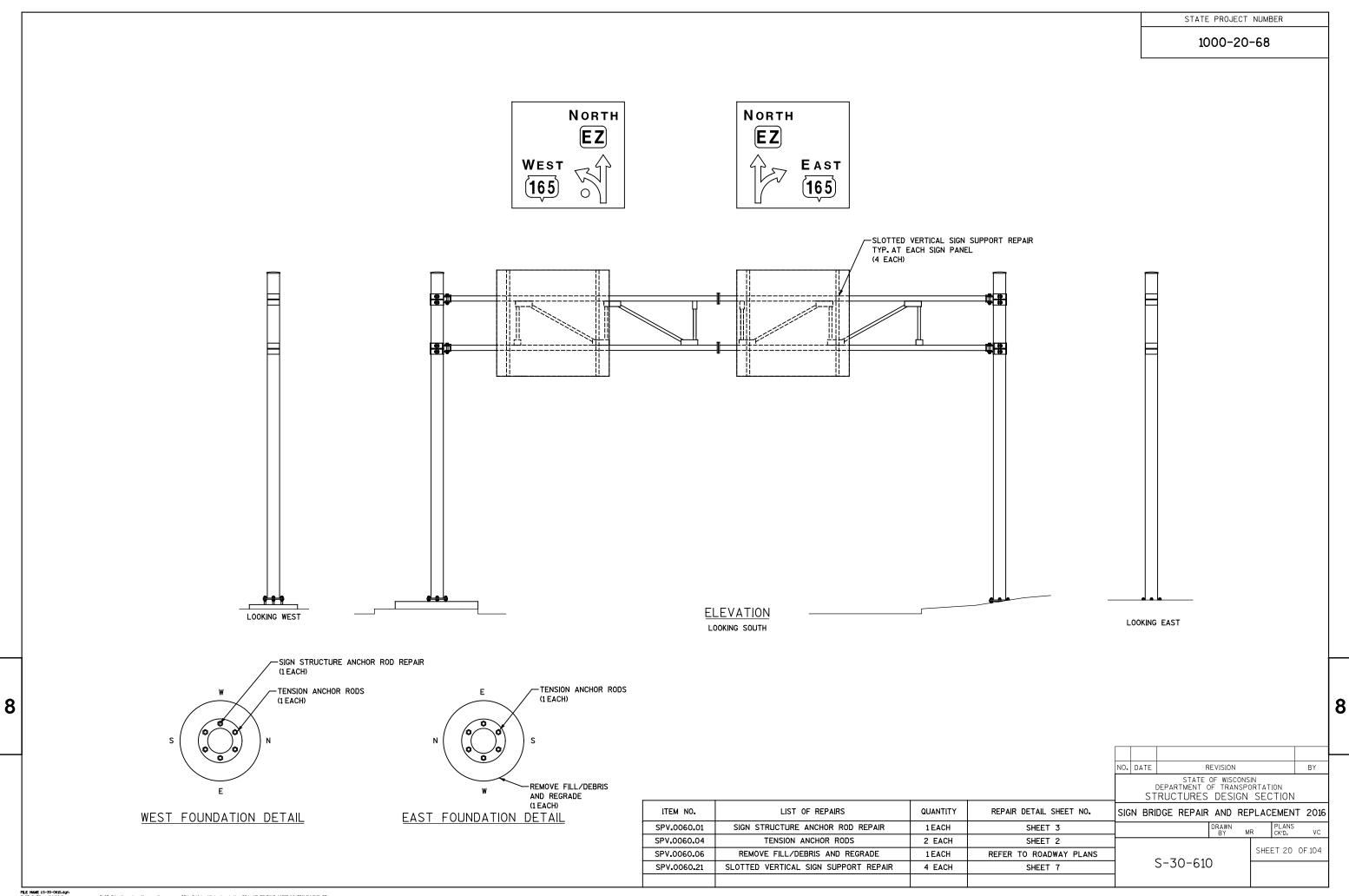


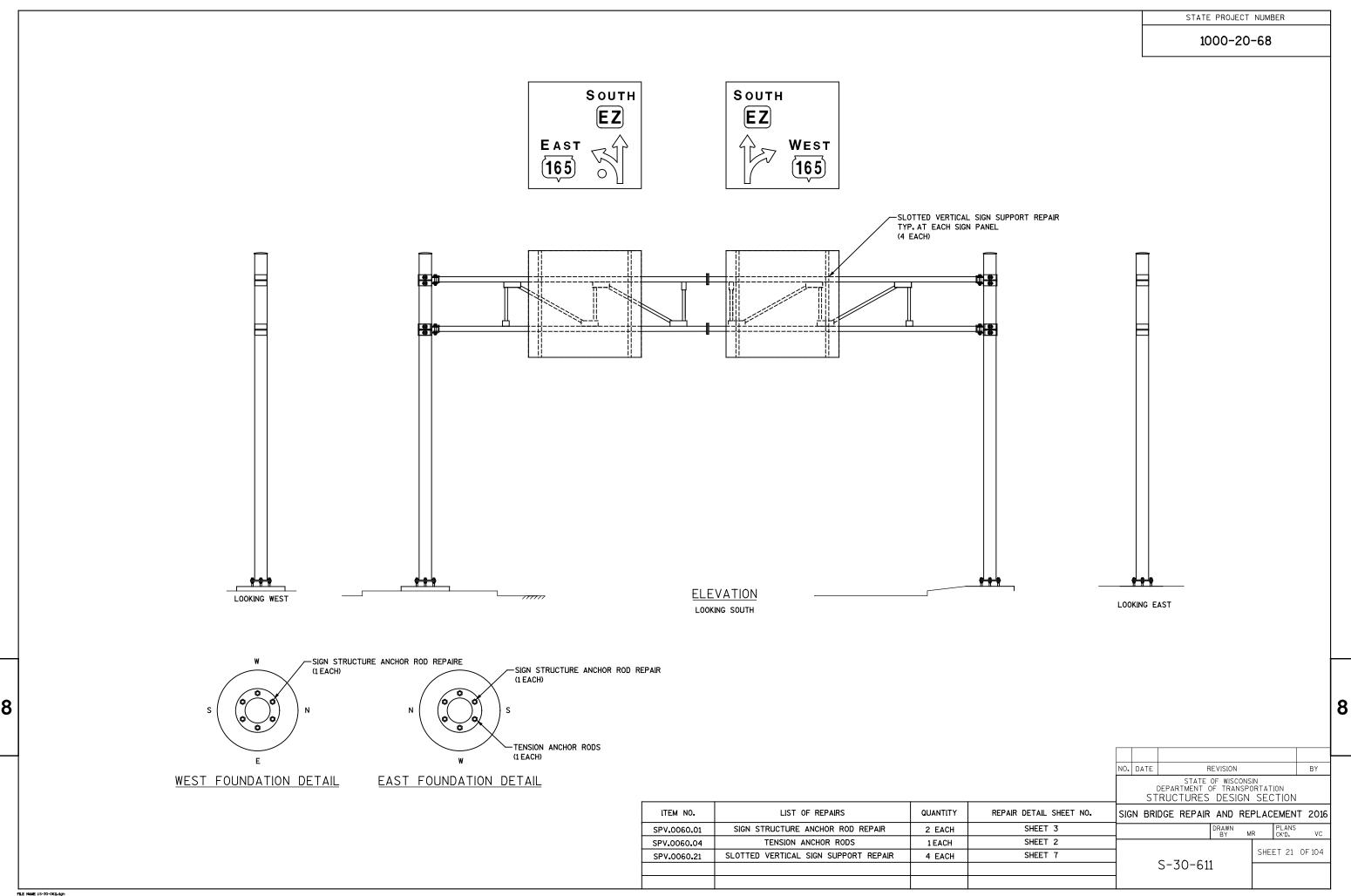


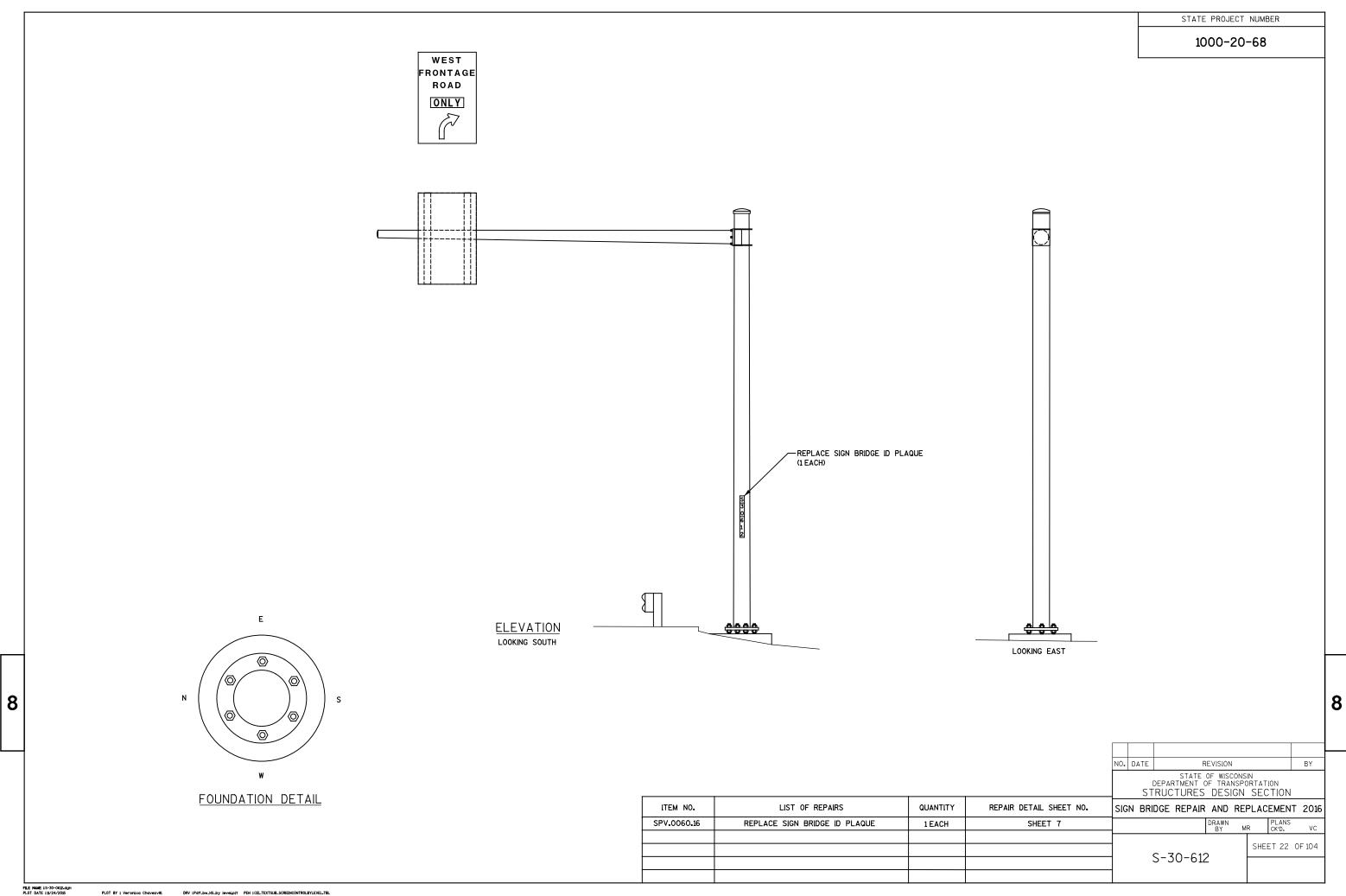
STATE PROJECT NUMBER 1000-20-68 V.M.S. REPLACE SIGN BRIDGE ID PLAQUE — (1 EACH) **ELEVATION** LOOKING NORTH LOOKING EAST 8 NO. DATE REVISION -REPLACE RODENT SCREEN (1 EACH) STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.03 1 EACH SHEET 2 REPLACE RODENT SCREEN SPV.0060.16 1 EACH SHEET 7 REPLACE SIGN BRIDGE ID PLAQUE SHEET 17 OF 104 S-30-255





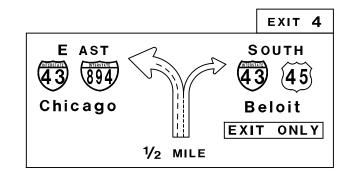


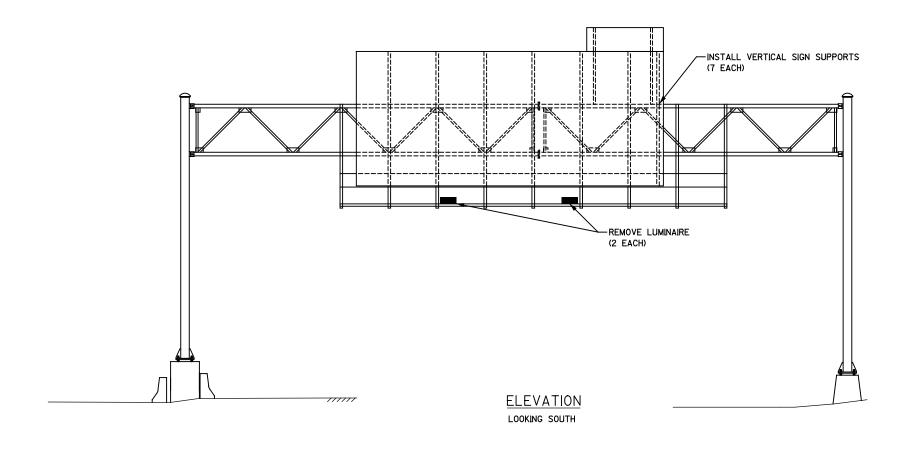


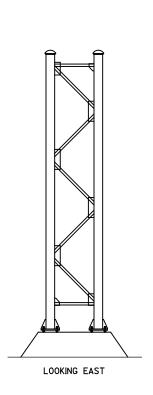


STATE PROJECT NUMBER 1000-20-68 EXIT 7 60 th Street REPLACE SIGN PANEL CONNECTOR-(16 EACH) 0 — RETAINING WALL **ELEVATION** LOOKING WEST LOOKING SOUTH 8 FOUNDATION DETAIL NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.17 REPLACE SIGN PANEL CONNECTOR 16 EACH SHEET 6 SHEET 23 OF 104 S-40-009

STATE PROJECT NUMBER 1000-20-68



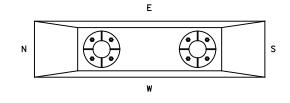






EAST FOUNDATION DETAIL

LOOKING WEST



WEST FOUNDATION DETAIL

NO.	DATE	REVISION	BY	
		STATE OF WISCONSIN		1

8

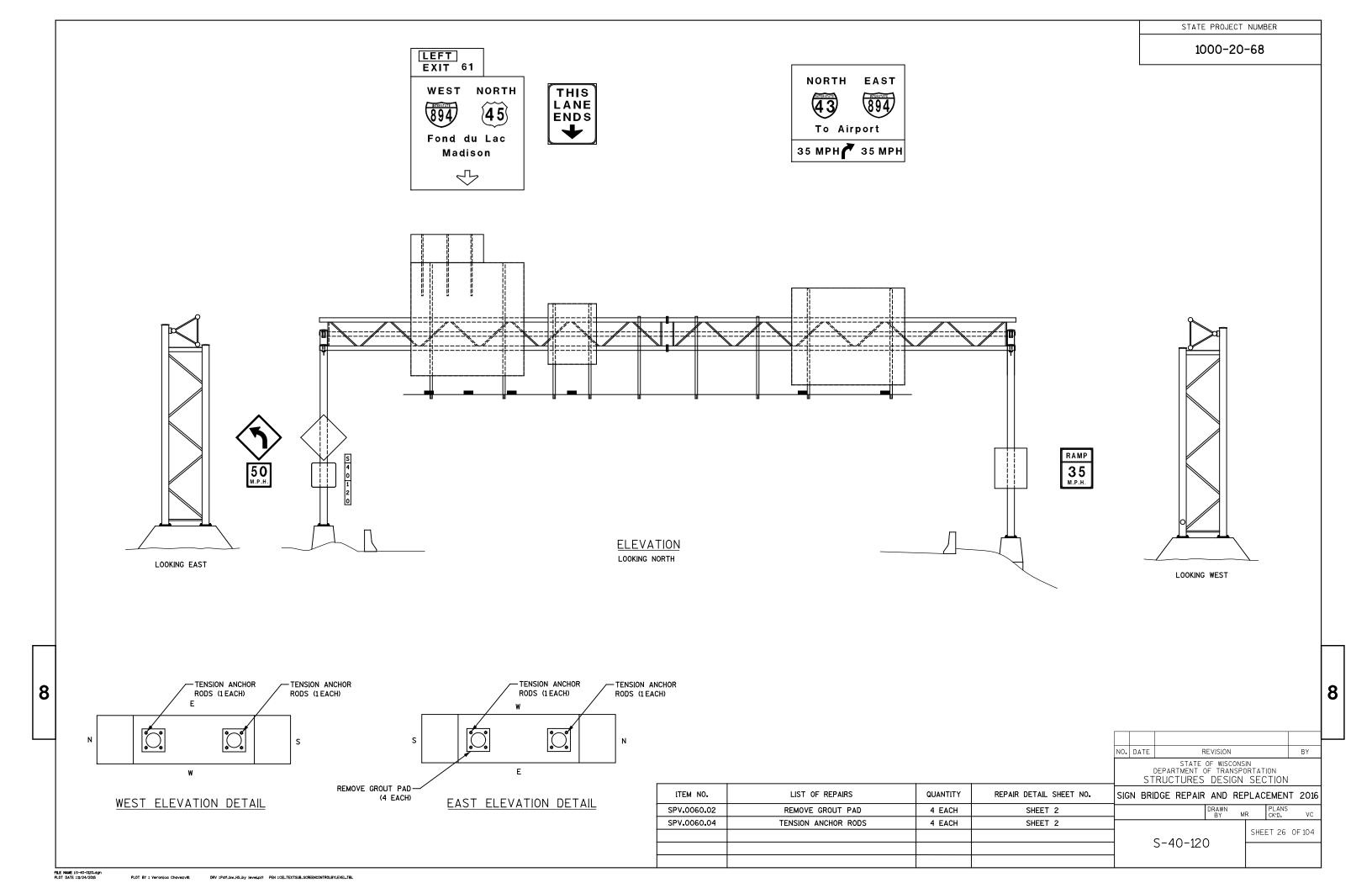
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

ITEM NO.	LIST OF REPAIRS	QUANTITY	REPAIR DETAIL SHEET NO.	SIGN BRIDGE REPAIR	AND F	REPLAC	CEMENT	2016
SPV.0060.22	INSTALL VERTICAL SIGN SUPPORT	7 EACH	SHEET 6		DRAWN BY	MR	PLANS CK'D.	VC
SPV.0060.29	REMOVE LUMINAIRE	2 EACH	SHEET 8				1	
				SHEET		ET 24 C	DF 104	
				S-40-018				
]				

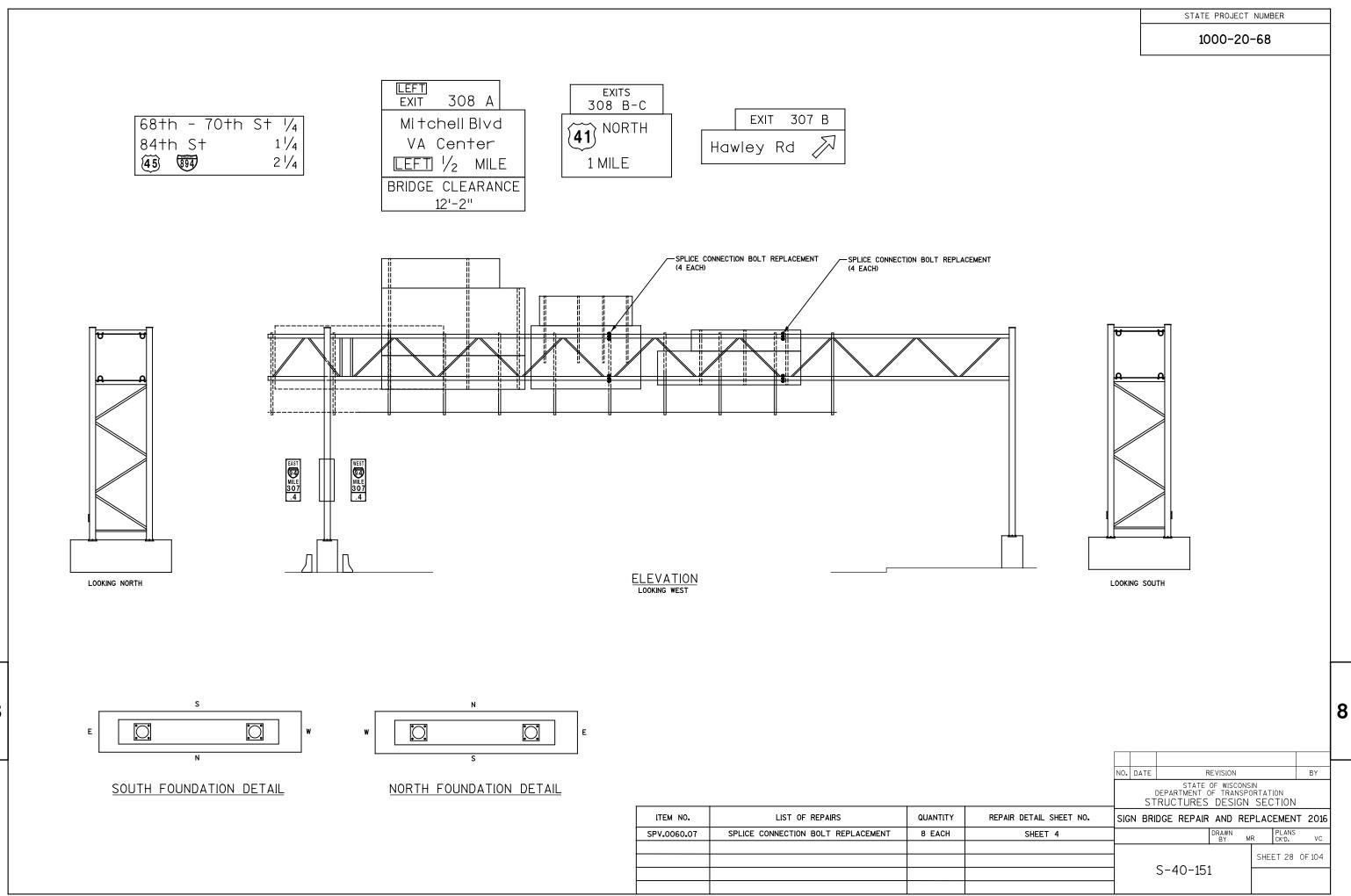
STATE PROJECT NUMBER 1000-20-68 (45) NORTH 100 NORTH TO (894) TO SOUTH 43 ELEVATION LOOKING EAST LOOKING NORTH LOOKING WEST TENSION ANCHOR RODS (1 EACH) TENSION ANCHOR RODS (1 EACH) TENSION ANCHOR RODS (1 EACH) TENSION ANCHOR 8 NO. DATE REVISION WEST FOUNDATION DETAIL EAST FOUNDATION DETAIL STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 4 EACH SHEET 2 SHEET 25 OF 104 S-40-049

ME :S-40-0049.dgh

ATE :11/24/2015 PLOT BY : Veronica Chavezv8l DRV :Pdf_bw_HS_by level.pit PEN :CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL



STATE PROJECT NUMBER 1000-20-68 EXIT 2 B Oklahoma Avenue **ELEVATION** LOOKING SOUTH LOOKING EAST - TENSION ANCHOR RODS 8 NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 1 EACH SHEET 2 SHEET 27 OF 104 S-40-126 PLOT BY : Veronica Chavezv8i DRV :Pdf_bw_HS_by level.pit PEN :CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL

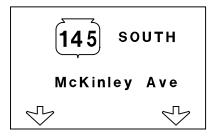


LE NAME: 15-40-0/5Ldgn
OT DATE: 11/24/20/5 PLOT BY: Veronica Chavezvël DRV: Pdf_bw_HS_by level.plt PEN: CEL.TEXTSUB_SCREENCONTROLBYLEVEL.TBL

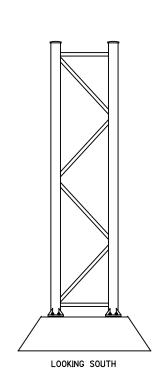
STATE PROJECT NUMBER

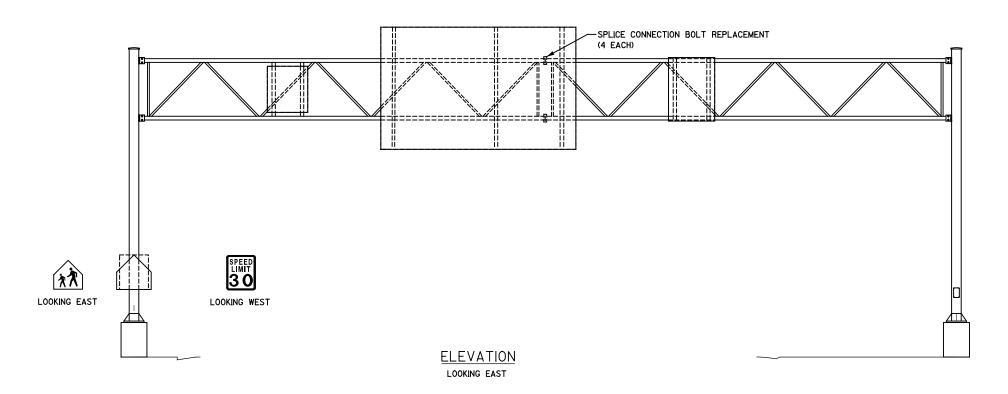
1000-20-68

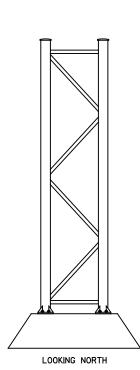




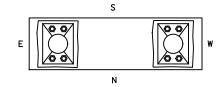




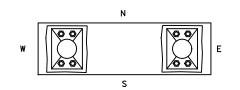




8



NORTH FOUNDATION DETAIL



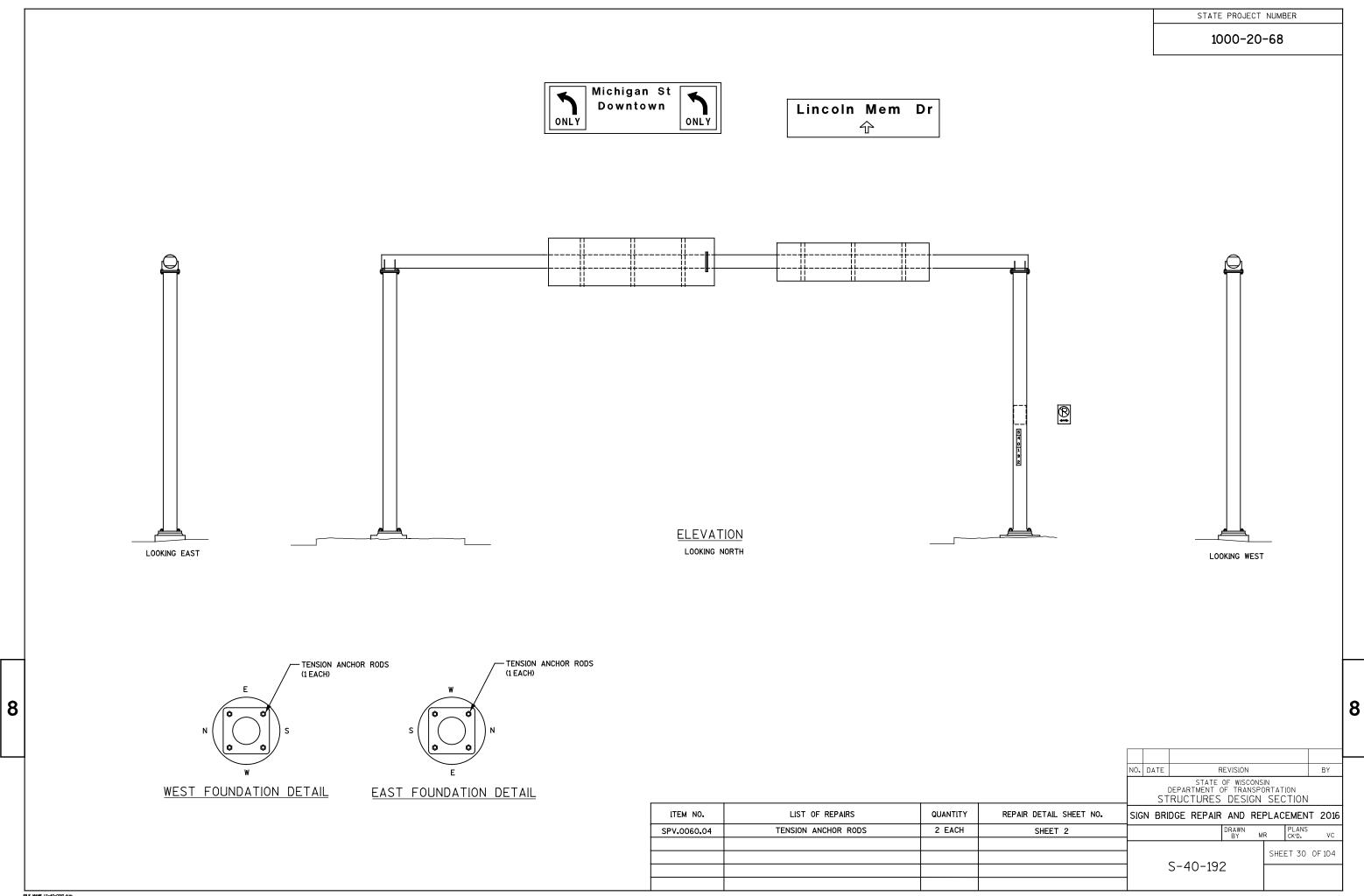
SOUTH FOUNDATION DETAIL

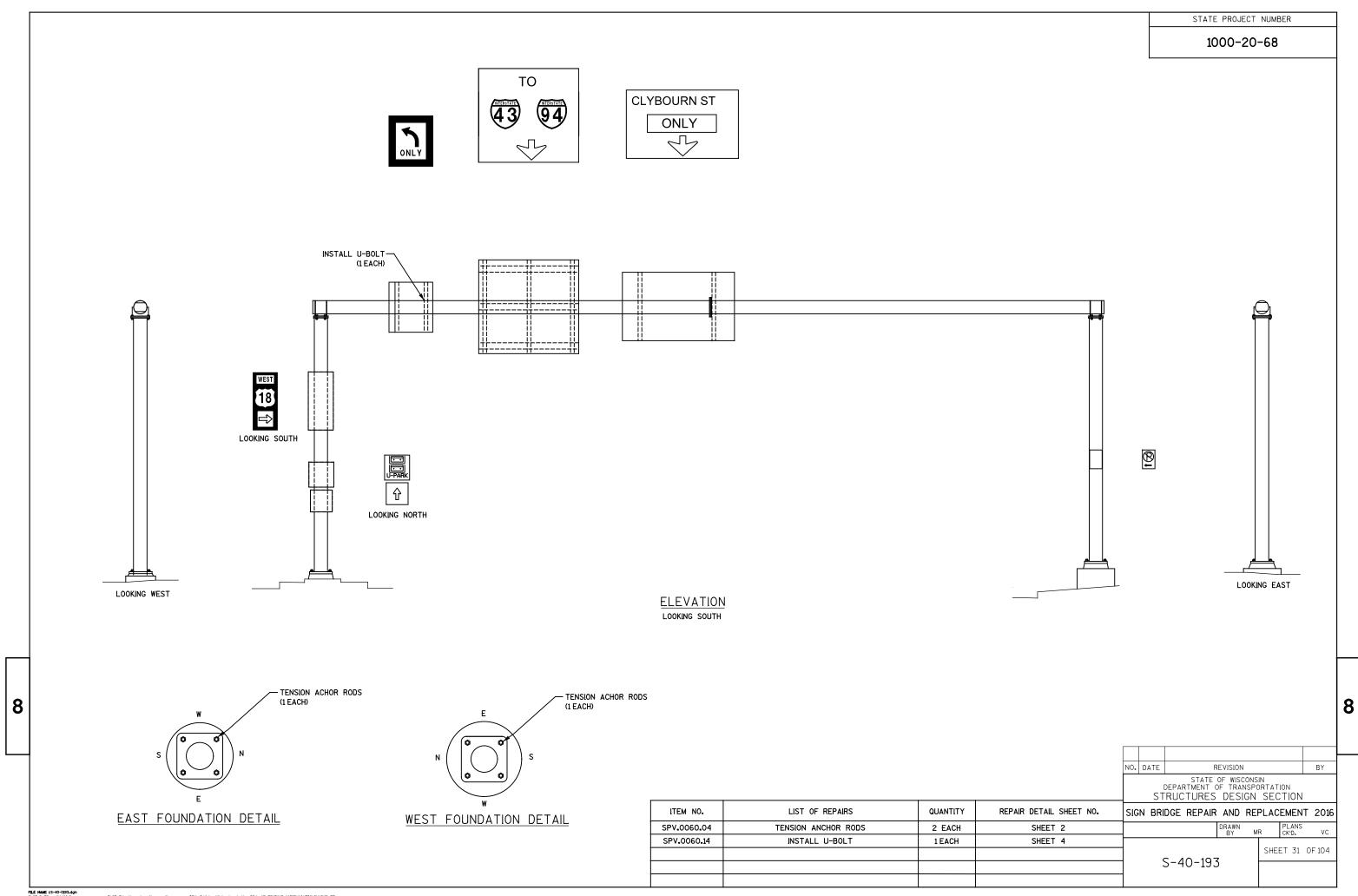
NO. DATE REVISION

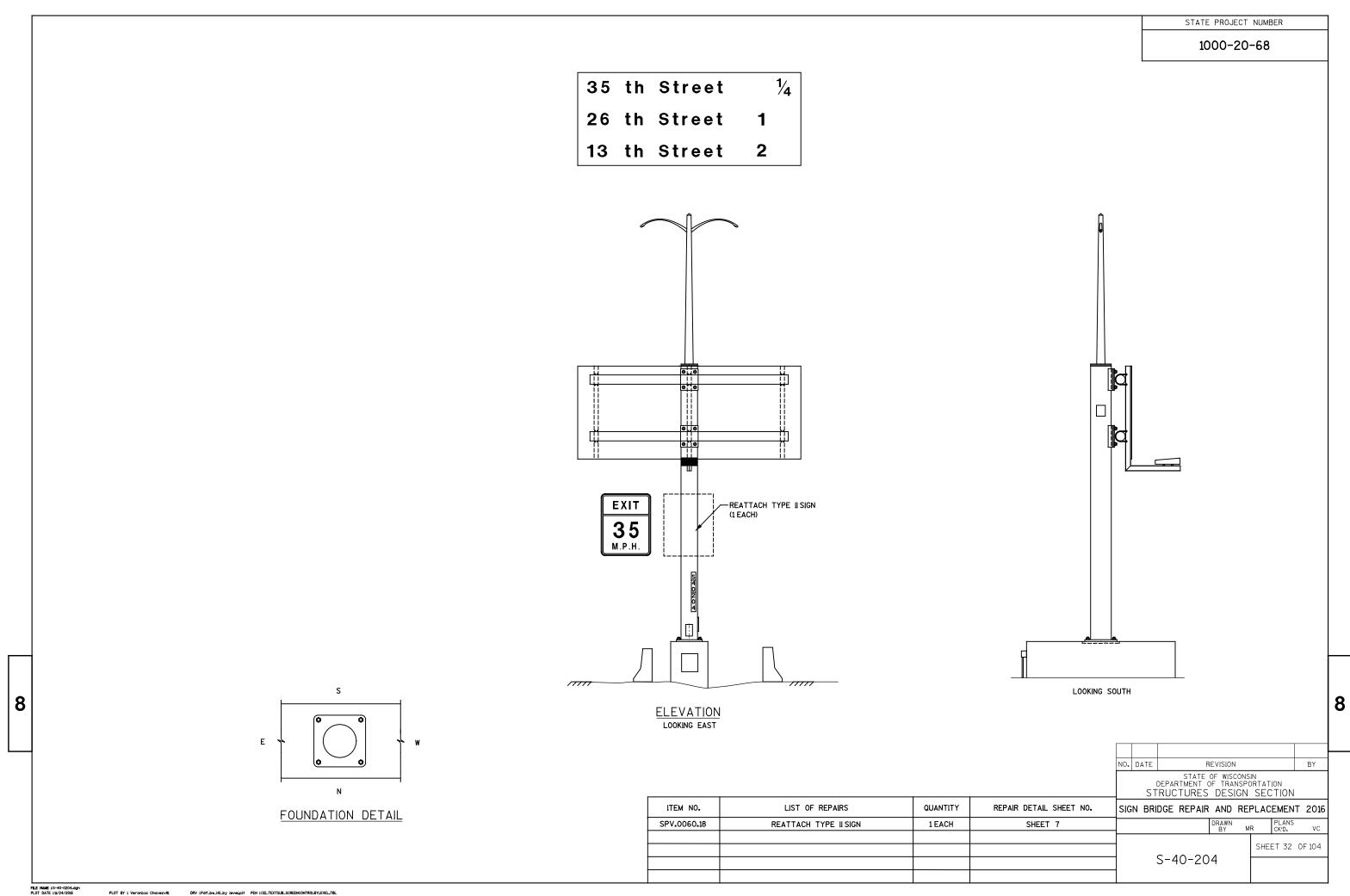
8

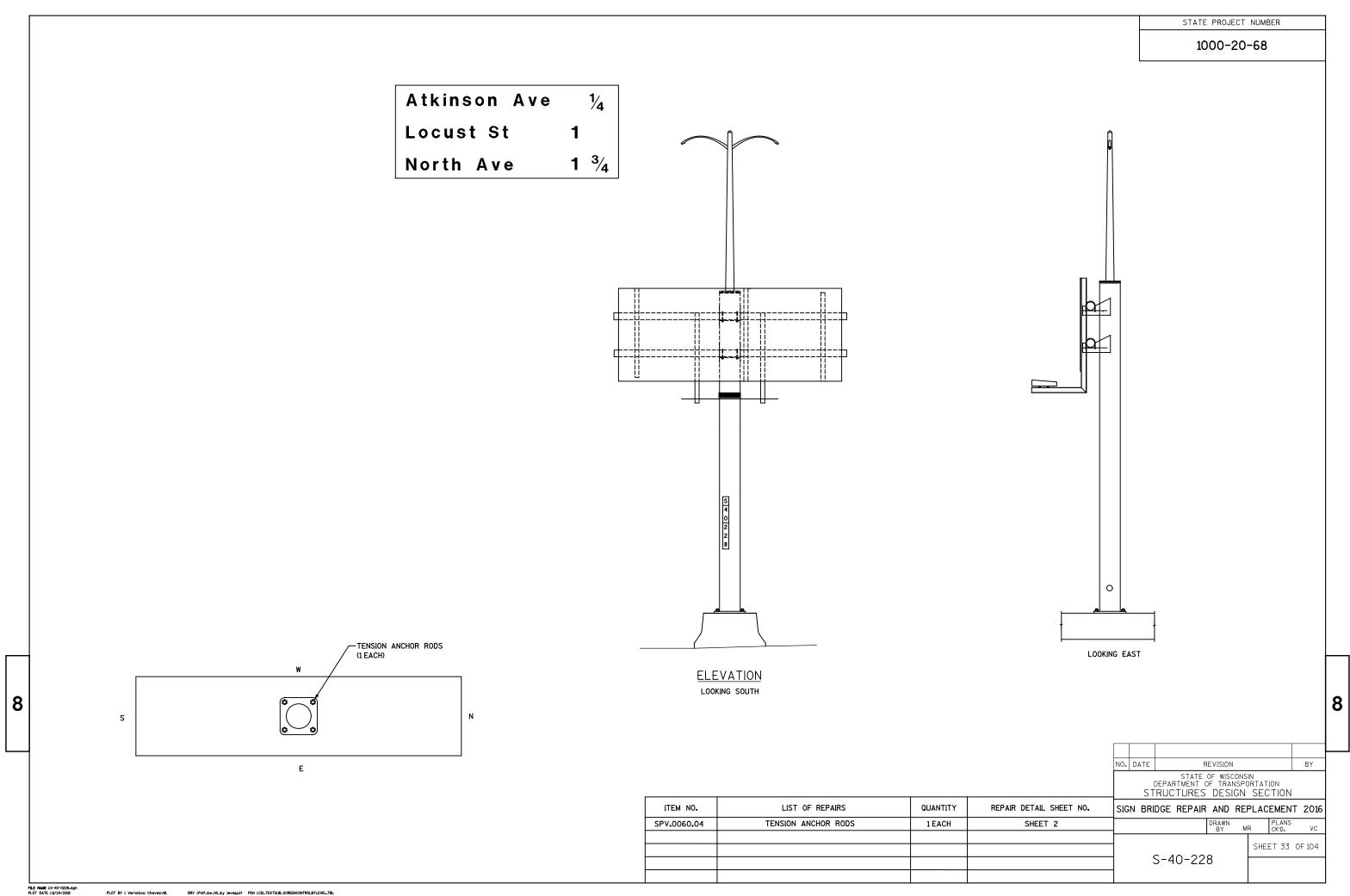
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

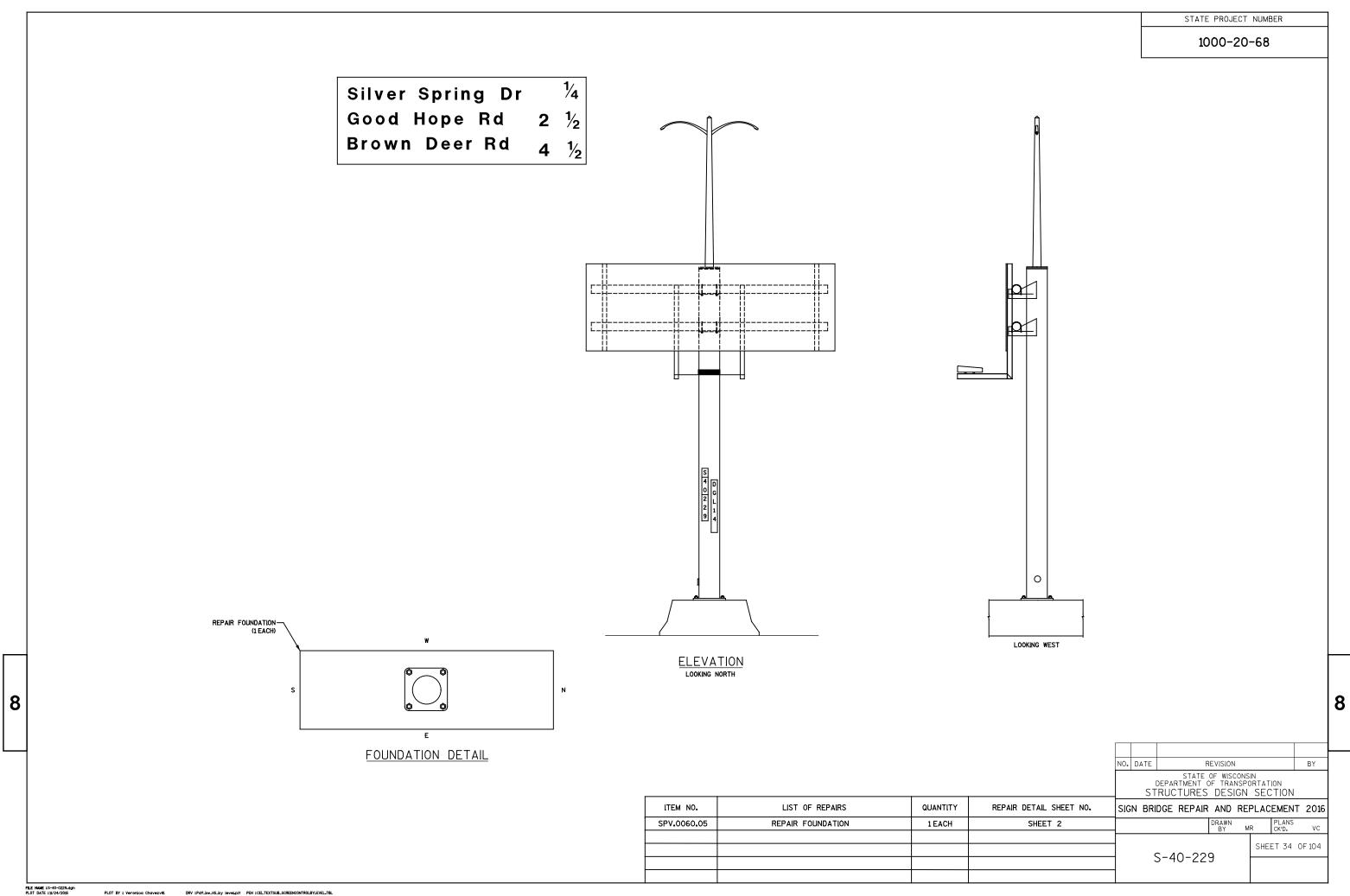
ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.07 SPLICE CONNECTION BOLT REPLACEMENT 4 EACH SHEET 4 SHEET 29 OF 104 S-40-185

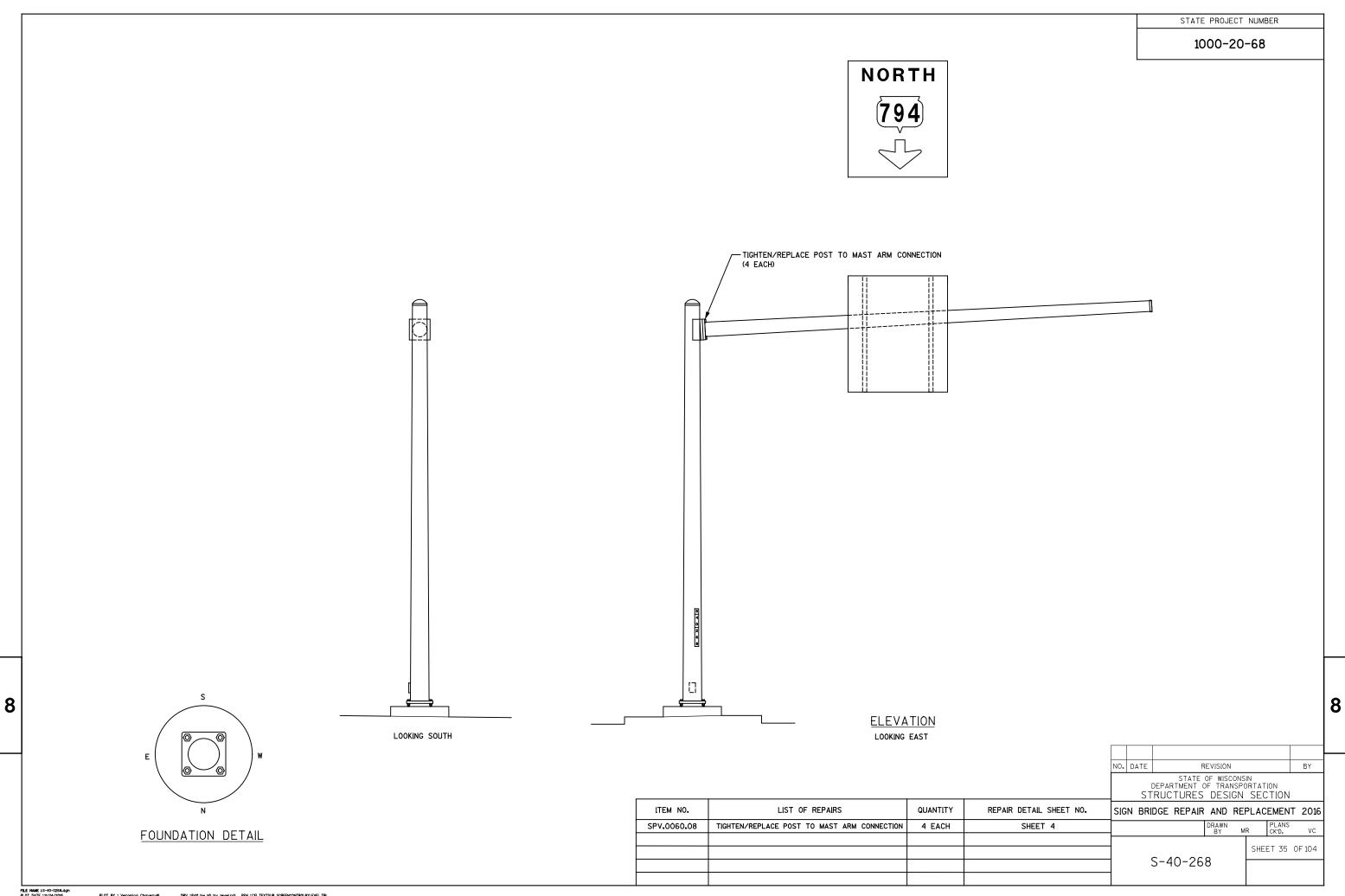


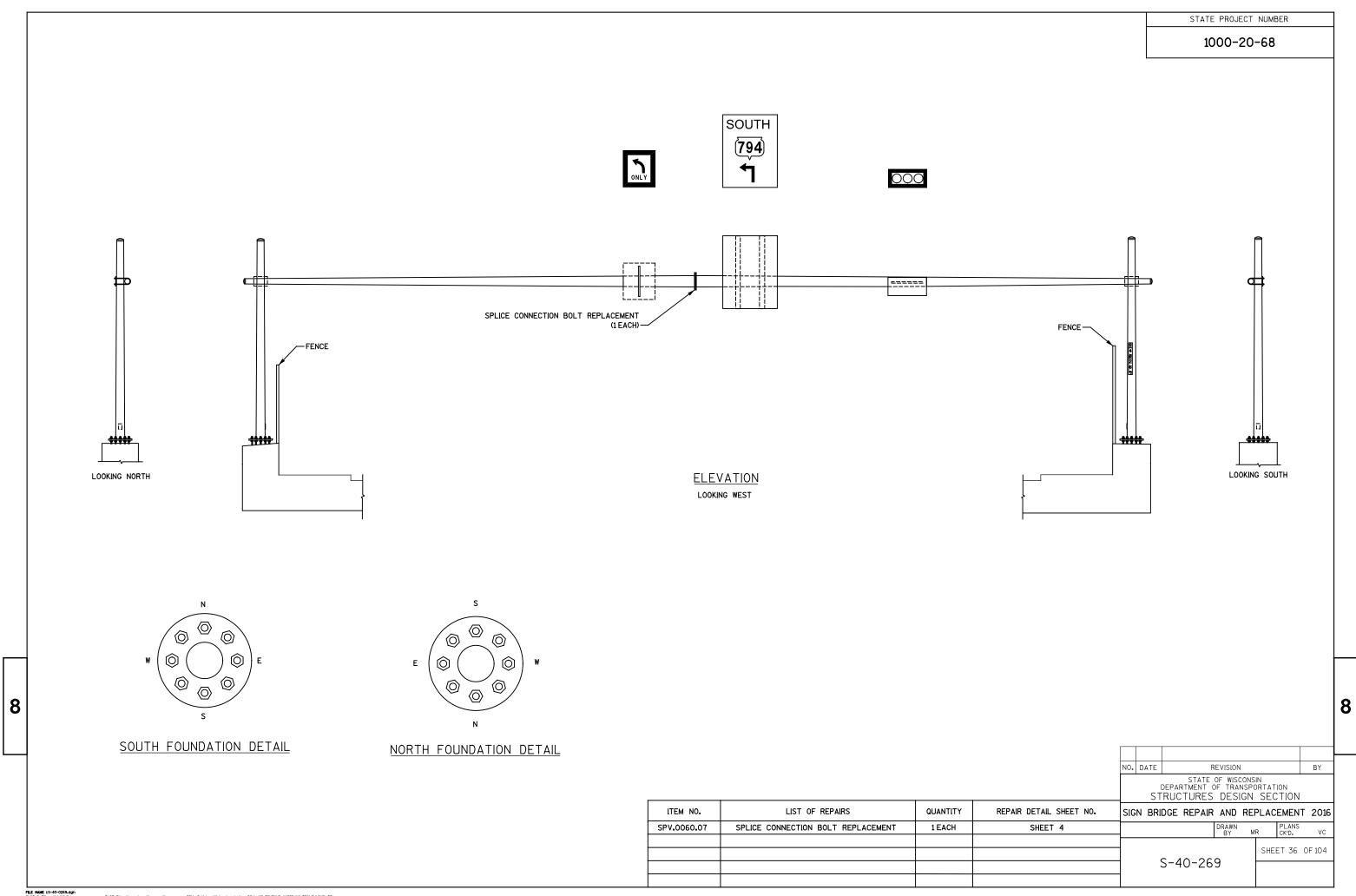


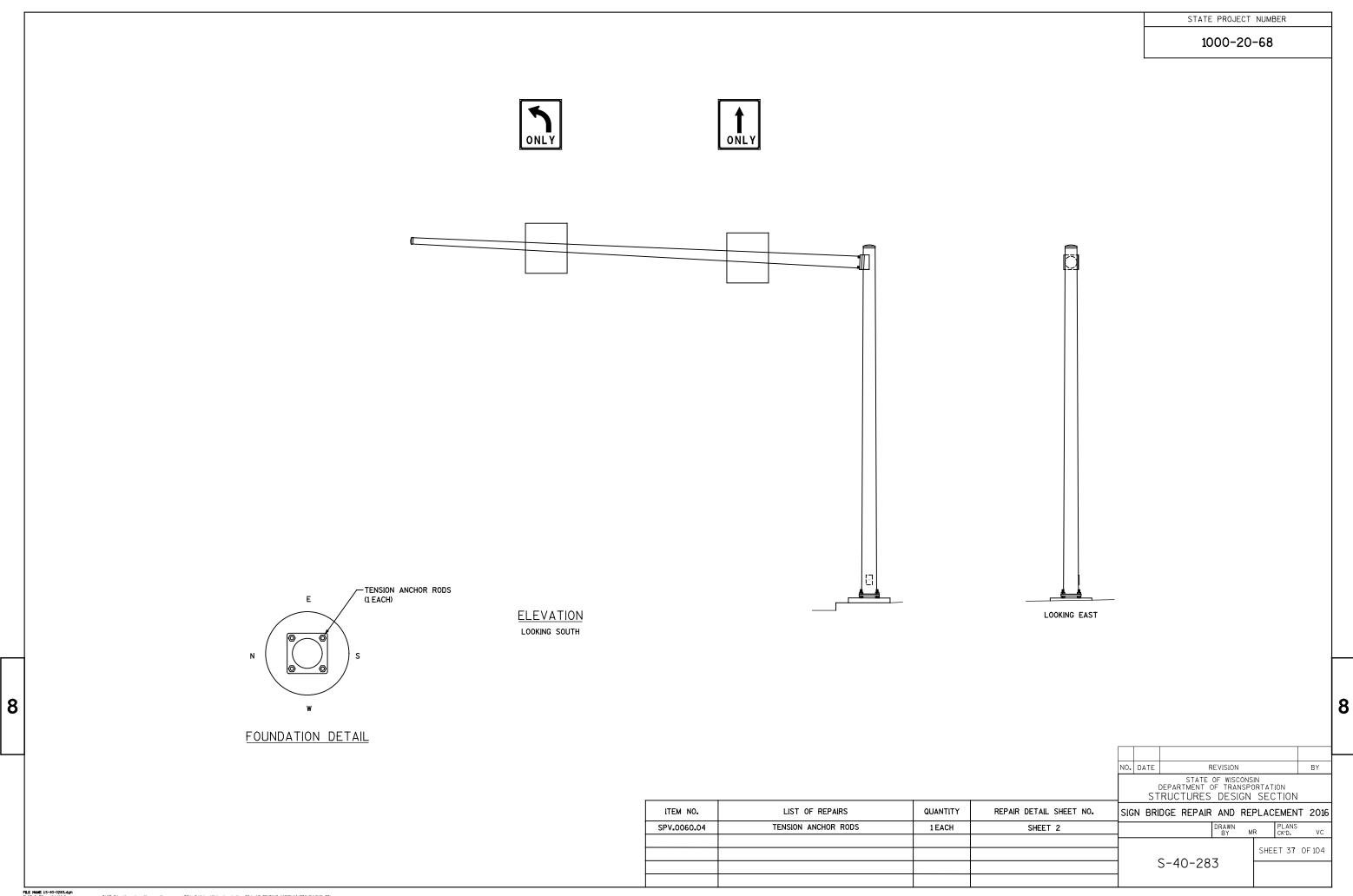


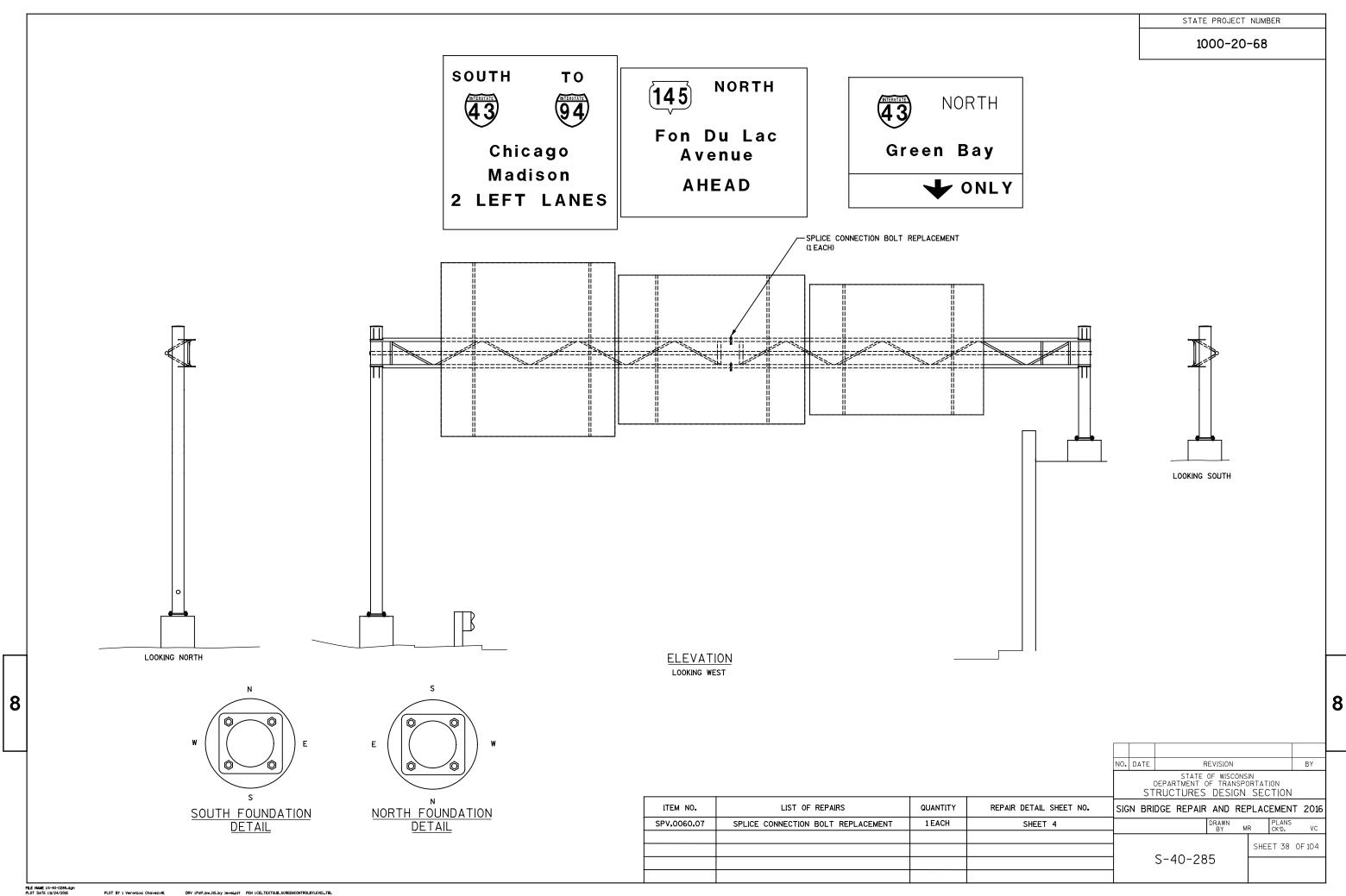








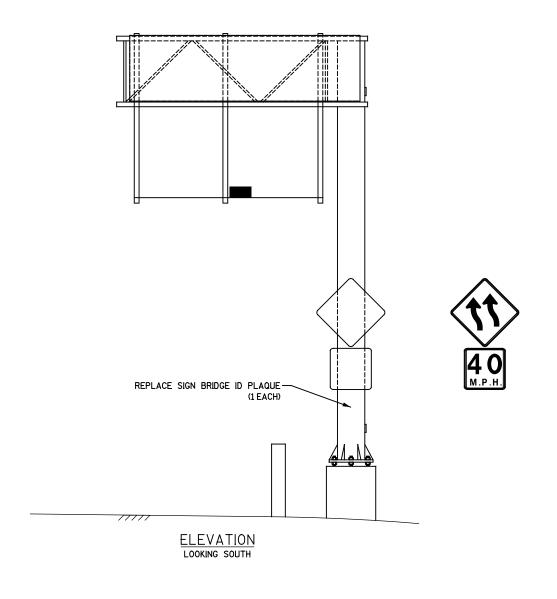


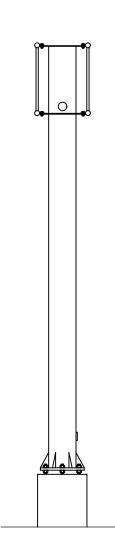


STATE PROJECT NUMBER

1000-20-68

Oklahoma Ave JCT LEFT 3/4 MILE





LOOKING EAST

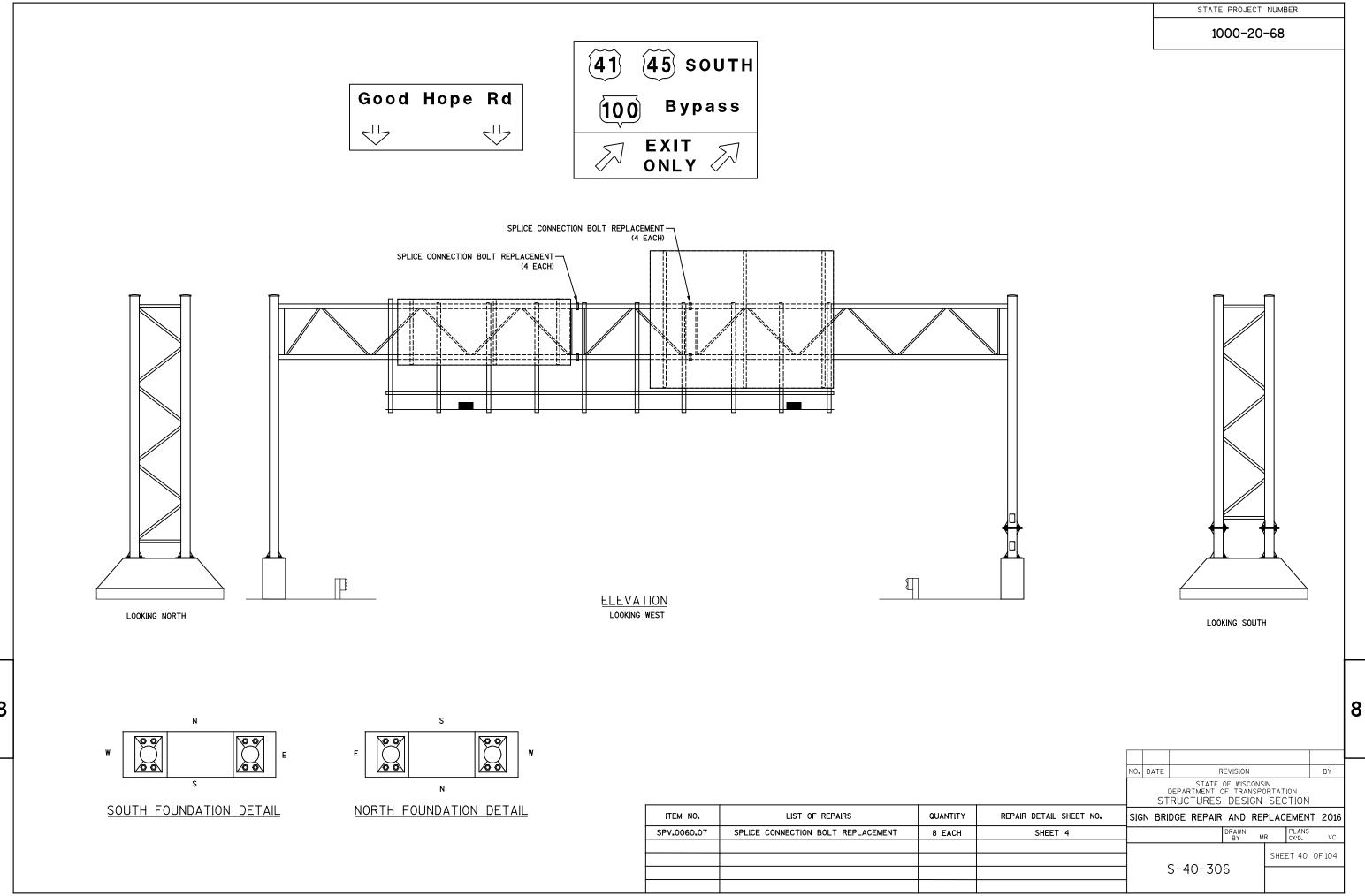
REPLACE RODENT SCREEN— (1 EACH) FOUNDATION DETAIL

NO. DATE REVISION

8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

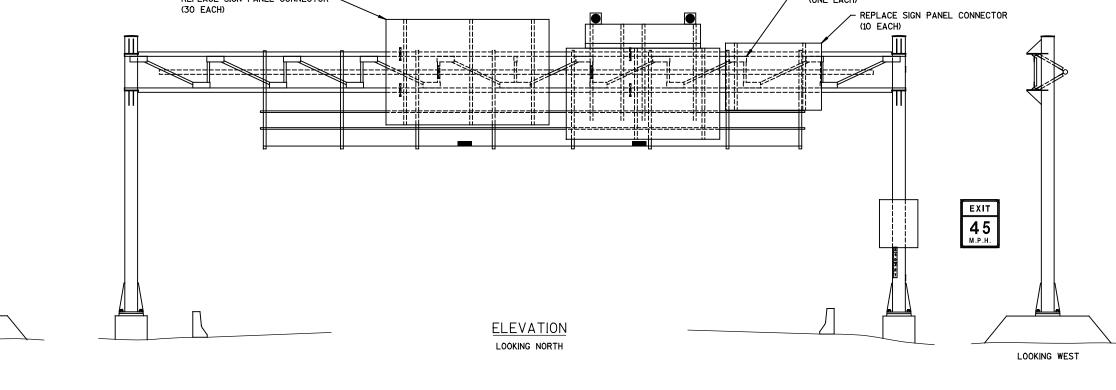
ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.03 REPLACE RODENT SCREEN 1 EACH SHEET 2 SPV.0060.16 1 EACH REPLACE SIGN BRIDGE ID PLAQUE SHEET 7 SHEET 39 OF 104 S-40-301

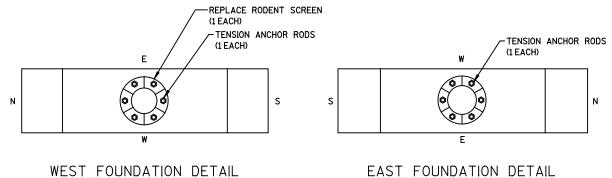


STATE PROJECT NUMBER 1000-20-68 VMS **(41)** THIS LANE ENDS SOUTH 45 100 Appleton Ave R **ELEVATION** LOOKING SOUTH LOOKING EAST LOOKING WEST TENSION ANCHOR RODS-(1 EACH) TENSION ANCHOR RODS-(1 EACH) 8 S NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION EAST FOUNDATION DETAIL WEST FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 2 EACH SHEET 2 SHEET 41 OF 104 S-40-326

NAME: S-40-0326.dgn
T DATE: 11/24/2015 PLOT BY: Veronlog Chavezv8L DRV: Pdf_bw_HS_by level.plt PEN: CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL

STATE PROJECT NUMBER 1000-20-68 VMS **175** W Metro Appleton Ave Auto Mall Fond du Lac R TENSION TRUSS CONNECTION BOLT (AT UF12) (ONE EACH) REPLACE SIGN PANEL CONNECTOR (30 EACH) - REPLACE SIGN PANEL CONNECTOR (10 EACH)





EAST FOUNDATION DETAIL

NO.	DATE	REVISION	BY	
		STATE OF WISCONSIN		l

8

DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

ITEM NO.	LIST OF REPAIRS	QUANTITY	REPAIR DETAIL SHEET NO.	SIGN BRIDGE REPAIR AND RE	PLACEMENT 2016
SPV.0060.03	REPLACE RODENT SCREEN	1 EACH	SHEET 2	DRAWN BY M	PLANS CK'D. VC
SPV.0060.04	TENSION ANCHOR RODS	2 EACH	SHEET 2		
SPV.0060.11	TENSION TRUSS CONNECTION BOLT	1 EACH	SHEET 4]	SHEET 42 OF 104
SPV.0060.17	REPLACE SIGN PANEL CONNECTOR	40 EACH	SHEET 6	S-40-327	
]	

8

LOOKING EAST

STATE PROJECT NUMBER 1000-20-68 (45) NORTH EXIT 10 C 145 NORTH Place 8 Fond du Lac ½ MILE EXIT ONLY —SECURE LUMINAIRE COVER (1 EACH) ELEVATION LOOKING WEST LOOKING EAST LOOKING NORTH - REPLACE RODENT SCREEN (1 EACH) -TENSION ANCHOR RODS -TENSION ANCHOR RODS (1 EACH) (1 EACH) 8 WEST FOUNDATION DETAIL EAST FOUNDATION DETAIL NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.03 REPLACE RODENT SCREEN SHEET 2 1 EACH TENSION ANCHOR RODS SPV.0060.04 SHEET 2 2 EACH SHEET 43 OF 104 SECURE LUMINAIRE COVER SPV.0060.24 1 EACH SHEET 8 S-40-328

STATE PROJECT NUMBER 1000-20-68 EXIT 45 Hampton Ave **EXIT ONLY** SECURE LUMINAIRE COVER (1 EACH) 0 ELEVATION LOOKING SOUTH 8 LOOKING EAST NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.24 SECURE LUMINAIRE COVER 1 EACH SHEET 8 SHEET 44 OF 104 S-40-352

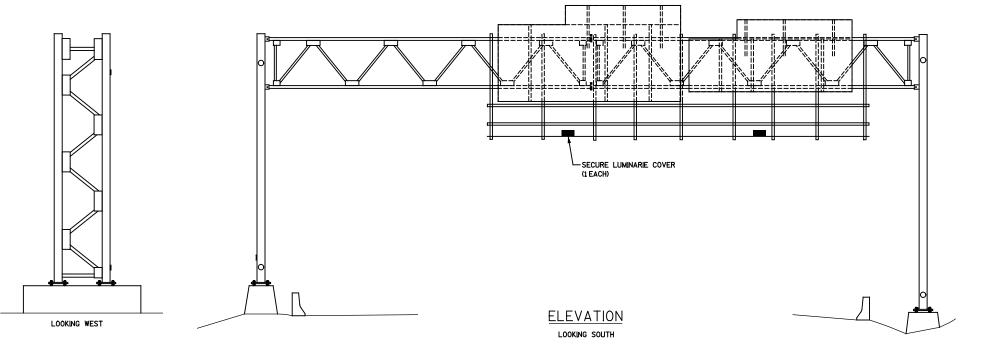
STATE PROJECT NUMBER 1000-20-68

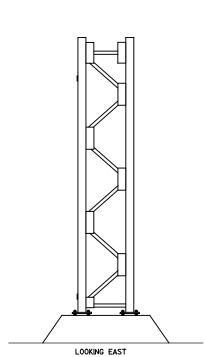
EXIT 42 A

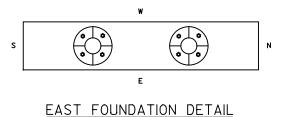
North Ave EAST Mayfair Rd $\frac{1}{4}$ MILE

EXIT 42 B









WEST FOUNDATION DETAIL

NO. DATE REVISION

8

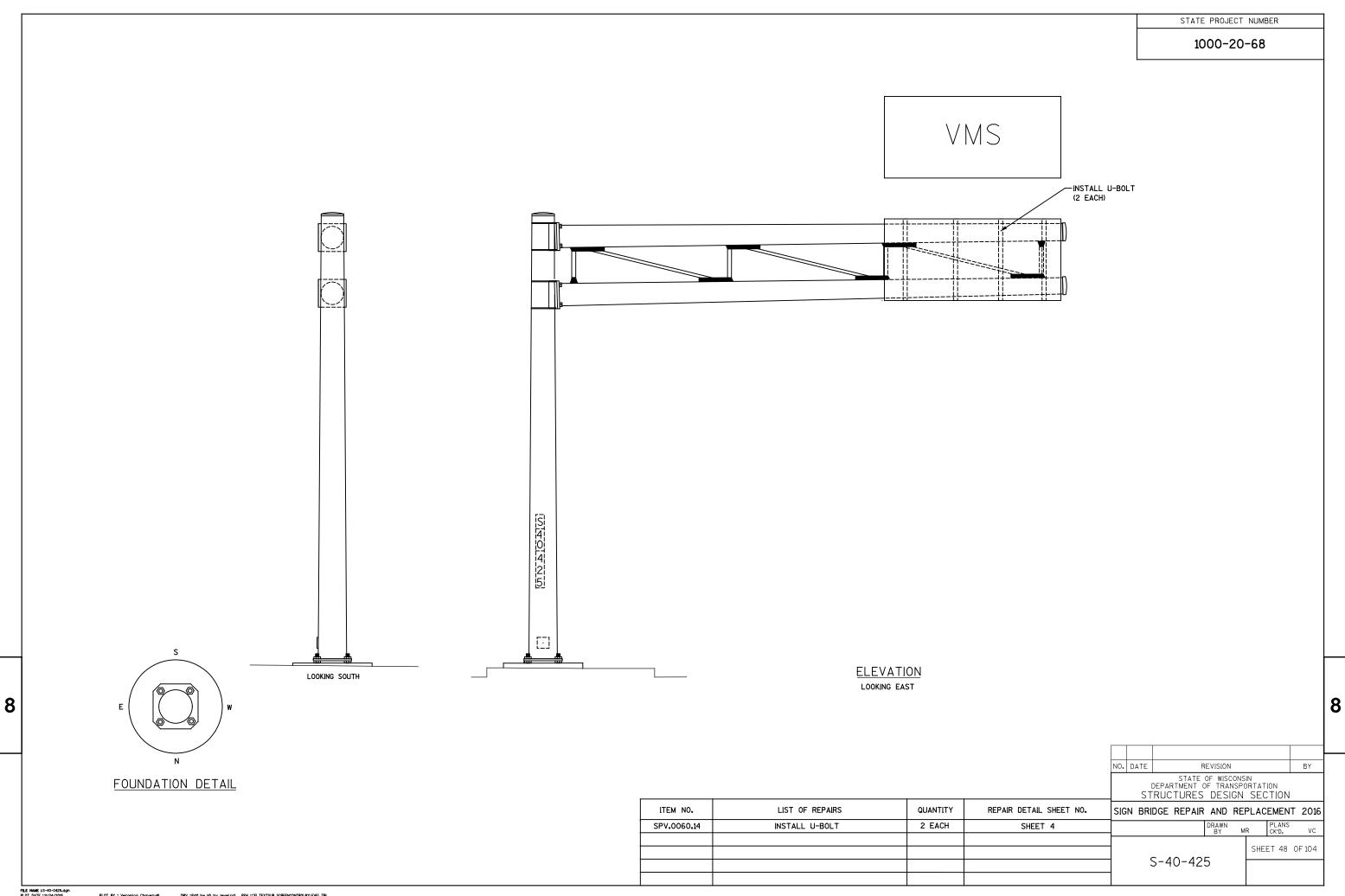
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION SIGN BRIDGE REPAIR AND REPLACEMENT 2016

ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SPV.0060.24 1 EACH SHEET 8 SECURE LUMINAIRE COVER SHEET 45 OF 104 S-40-355

STATE PROJECT NUMBER 1000-20-68 **EXIT** 312 A **EXIT 312 B** Lapham Blvd Becher St Mitchell St Lincoln Ave $\frac{1}{4}$ MILE **EXIT ONLY** REMOVE ASIM TRAFFIC COUNTER UNITS (4 EACH)* **ELEVATION** LOOKING EAST LOOKING WEST LOOKING SOUTH 8 NO. DATE STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION *AISM TRAFFIC COUNTERS TO BE TURNED OVER TO BRIAN SCHARLES, TAPCO REPAIR DETAIL SHEET NO. ITEM NO. LIST OF REPAIRS QUANTITY SIGN BRIDGE REPAIR AND REPLACEMENT 2016 EAST FOUNDATION DETAIL WEST FOUNDATION DETAIL SPV.0060.28 REMOVE ASIM TRAFFIC COUNTER UNITS 4 EACH SHEET 8 SHEET 46 OF 104 S-40-393

NAME:S-40-0393.dgh
T DATE: III/24/2015 PLOT BY: Veronloa Chavezv8L DRV: Pdf_bw_HS_by_level.plt PEN:CELTextSub_ScreenControlByLevel.tbl

STATE PROJECT NUMBER 1000-20-68 **VMS** REPLACE CONDUIT PLUG (1 EACH) DO NOT ENTER **ELEVATION** LOOKING SOUTH LOOKING EAST 8 - REPLACE RODENT SCREEN (1 EACH) NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.03 REPLACE RODENT SCREEN 1 EACH SHEET 2 REPLACE CONDUIT PLUG 1 EACH SHEET 8 SPV.0060.27 SHEET 47 OF 104 S-40-423 PLOT BY : Veronica Chavezv8i DRV :Pdf_bw_HS_by level.pit PEN :CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL



STATE PROJECT NUMBER 1000-20-68 **VMS** - SPLICE CONNECTION BOLT REPLACEMENT (2 EACH) **ELEVATION** LOOKING WEST LOOKING SOUTH LOOKING EAST 8 NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION EAST FOUNDATION DETAIL WEST FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.07 SPLICE CONNECTION BOLT REPLACEMENT 2 EACH SHEET 4 SHEET 49 OF 104 S-40-426 PLOT BY : Veronica Chavezv8i DRV :Pdf_bw_HS_by level.pit PEN :CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL

STATE PROJECT NUMBER 1000-20-68 TUNNEL AHEAD VMS LOOKING NORTH **ELEVATION** LOOKING SOUTH LOOKING EAST -TENSION ANCHOR RODS 8 - REMOVE FILL/DEBRIS AND REGRADE (1 EACH) NORTH FOUNDATION DETAIL SOUTH FOUNDATION DETAIL NO. DATE STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 1 EACH SHEET 2 REMOVE FILL/DEBRIS AND REGRADE REFER TO ROADWAY PLANS SPV.0060.06 SHEET 50 OF 104 S-40-436

STATE PROJECT NUMBER 1000-20-68 TUNNEL AHEAD VMS **ELEVATION** LOOKING SOUTH LOOKING NORTH LOOKING EAST TENSION ANCHOR RODS (1 EACH) TENSION ANCHOR RODS (1 EACH) 8 NORTH FOUNDATION DETAIL SOUTH FOUNATION DETAIL NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 2 EACH SHEET 2 SHEET 51 OF 104 S-40-437

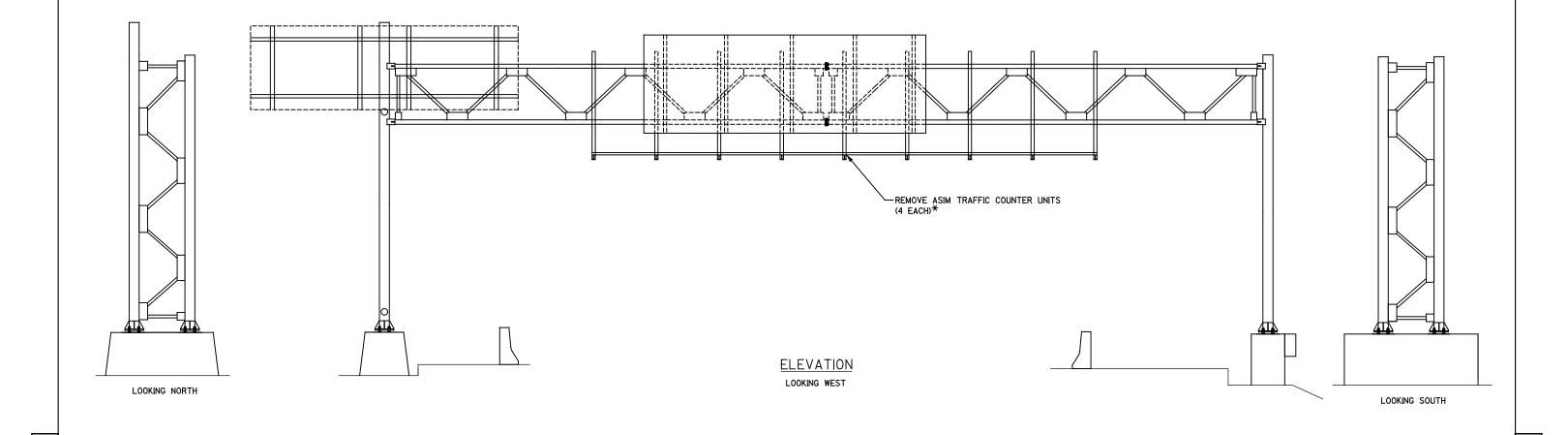
STATE PROJECT NUMBER 1000-20-68 TUNNEL TUNNEL VMS |VMS| VMS AHEAD AHEAD SPEED LIMIT **50** -REPLACE SIGN BRIDGE ID PLAQUE (1 EACH) ELEVATION LOOKING EAST LOOKING NORTH LOOKING WEST -REPLACE RODENT SCREEN Ε (1 EACH) 8 NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION WEST FOUNDATION DETAIL EAST FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SHEET 2 SPV.0060.03 1 EACH REPLACE RODENT SCREEN SPV.0060.16 1 EACH SHEET 7 REPLACE SIGN BRIDGE ID PLAQUE SHEET 52 OF 104 S-40-438

STATE PROJECT NUMBER

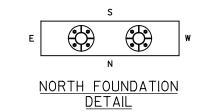
1000-20-68

Hawley Rd Mitchell Blvd LEFT Miller Park Way 1 1/4

V.M.S.



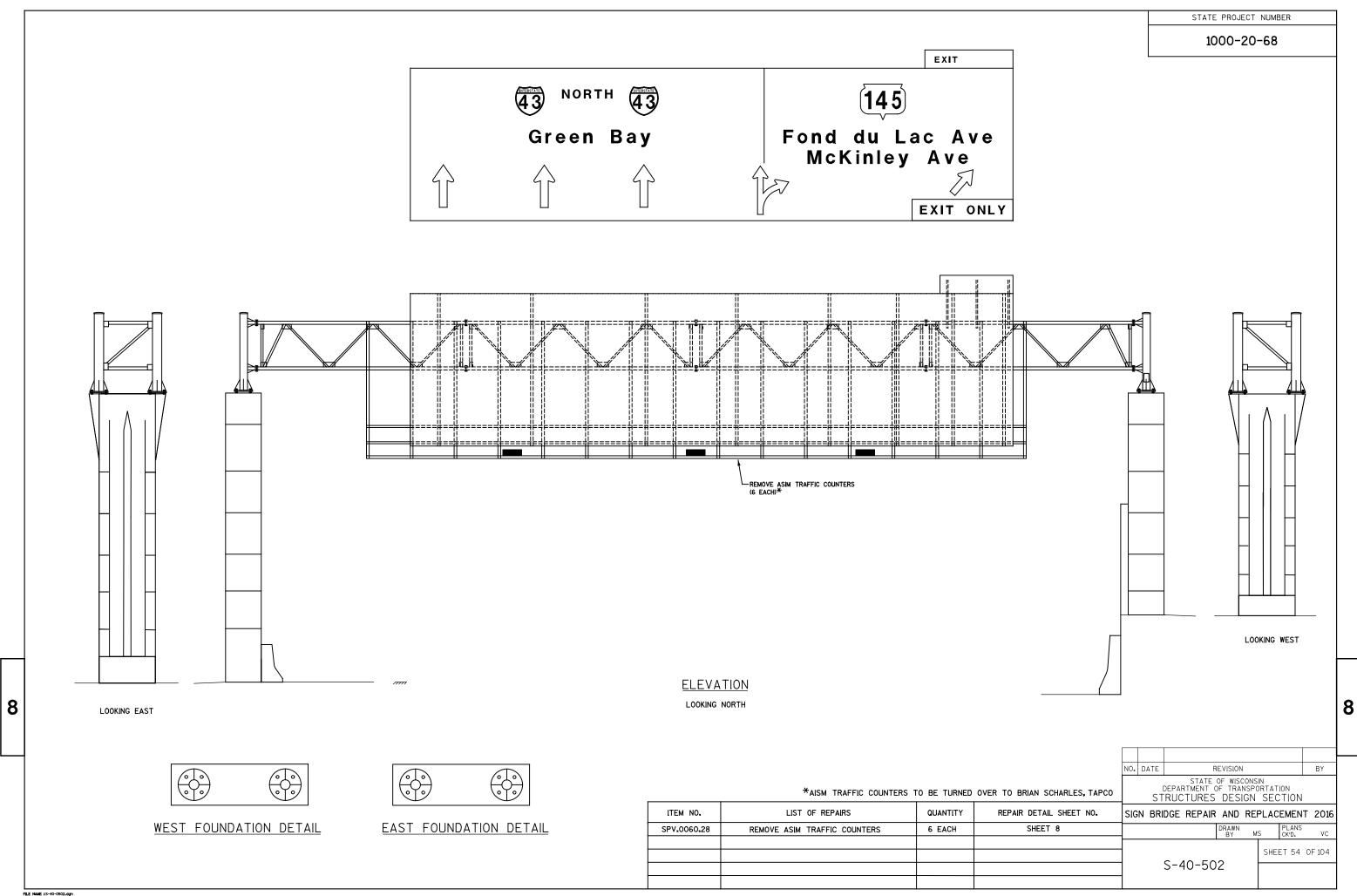




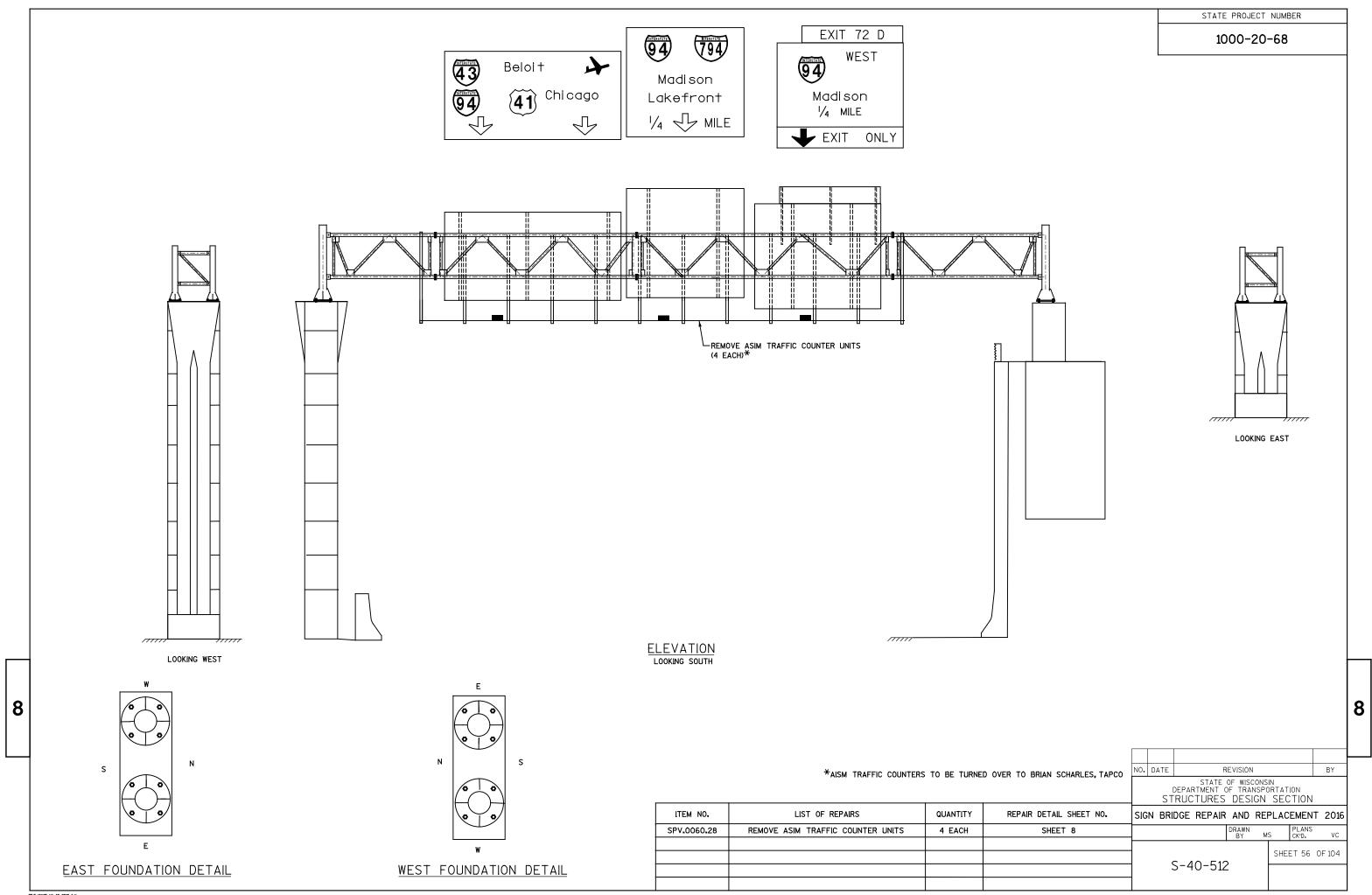
*AISM TRAFFIC COUNTERS TO BE TURNED OVER TO BRIAN SCHARLES, TAPCO

NO.	DATE	REVISION	BY	
		STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION RUCTURES DESIGN SECTION		

					11.00101				
ITEM NO.	LIST OF REPAIRS	QUANTITY	REPAIR DETAIL SHEET NO.	SIGN BRI	IDGE REP	AIR AND	REPLA	CEMENT	2016
SPV.0060.28	REMOVE ASIM TRAFFIC COUNTER UNITS	4	SHEET 8			DRAWN	MR	PLANS CK'D.	VC
				S-40-443					
						IEET 53 ()F 104		



STATE PROJECT NUMBER 1000-20-68 145 NORTH 145 SOUTH Fond du Lac McKinley Avenue Avenue — SECURE LUMINAIRE COVER (1 EACH) LOOKING EAST **ELEVATION** LOOKING NORTH 8 8 NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.24 SECURE LUMINAIRE COVER 1 EACH SHEET 8 SHEET 55 OF 104 S-40-503 PLOT BY: Veronica Chavezv8i DRV: Pdf_bw_HS_by level.pit PEN: CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL

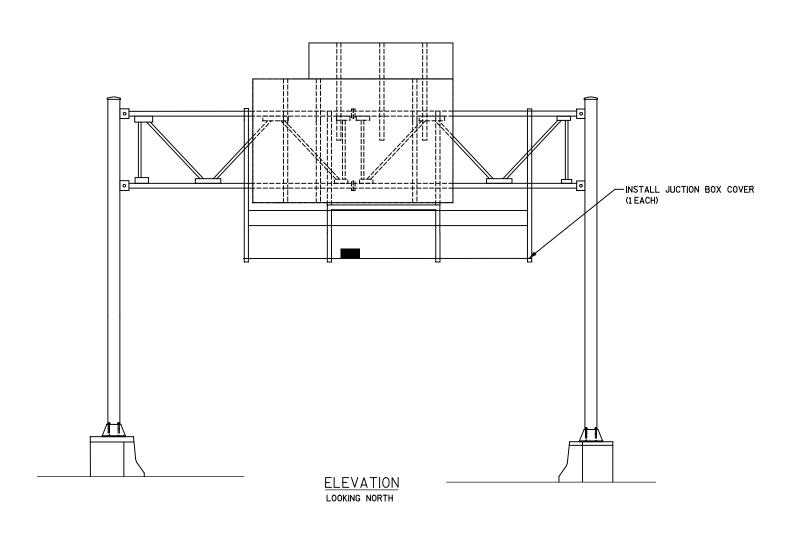


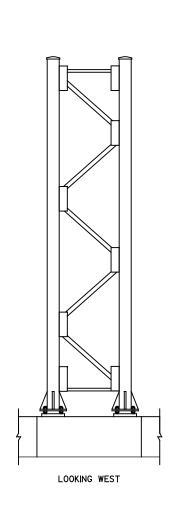
STATE PROJECT NUMBER

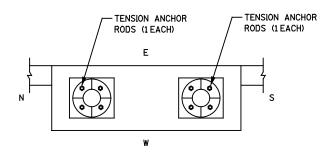
1000-20-68

EXIT 1D

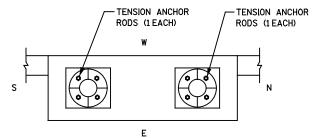
Plankinton Avenue **NEXT RIGHT**







WEST FOUNDATION DETAIL



EAST FOUNDATION DETAIL

				STRUCTURES DESIGN SECTION				
ITEM NO.	LIST OF REPAIRS	QUANTITY	REPAIR DETAIL SHEET NO.	SIGN BRIDGE REPAIR	AND R	EPLAC	EMENT	2016
SPV.0060.04	TENSION ANCHOR RODS	4 EACH	SHEET 2	C	RAWN BY	MR	PLANS CK'D.	VC
SPV.0060.25	INSTALL JUNCTION BOX COVER	1 EACH	SHEET 8					
				S-40-521		SHE	SHEET 57 OF 10	

NO. DATE REVISION

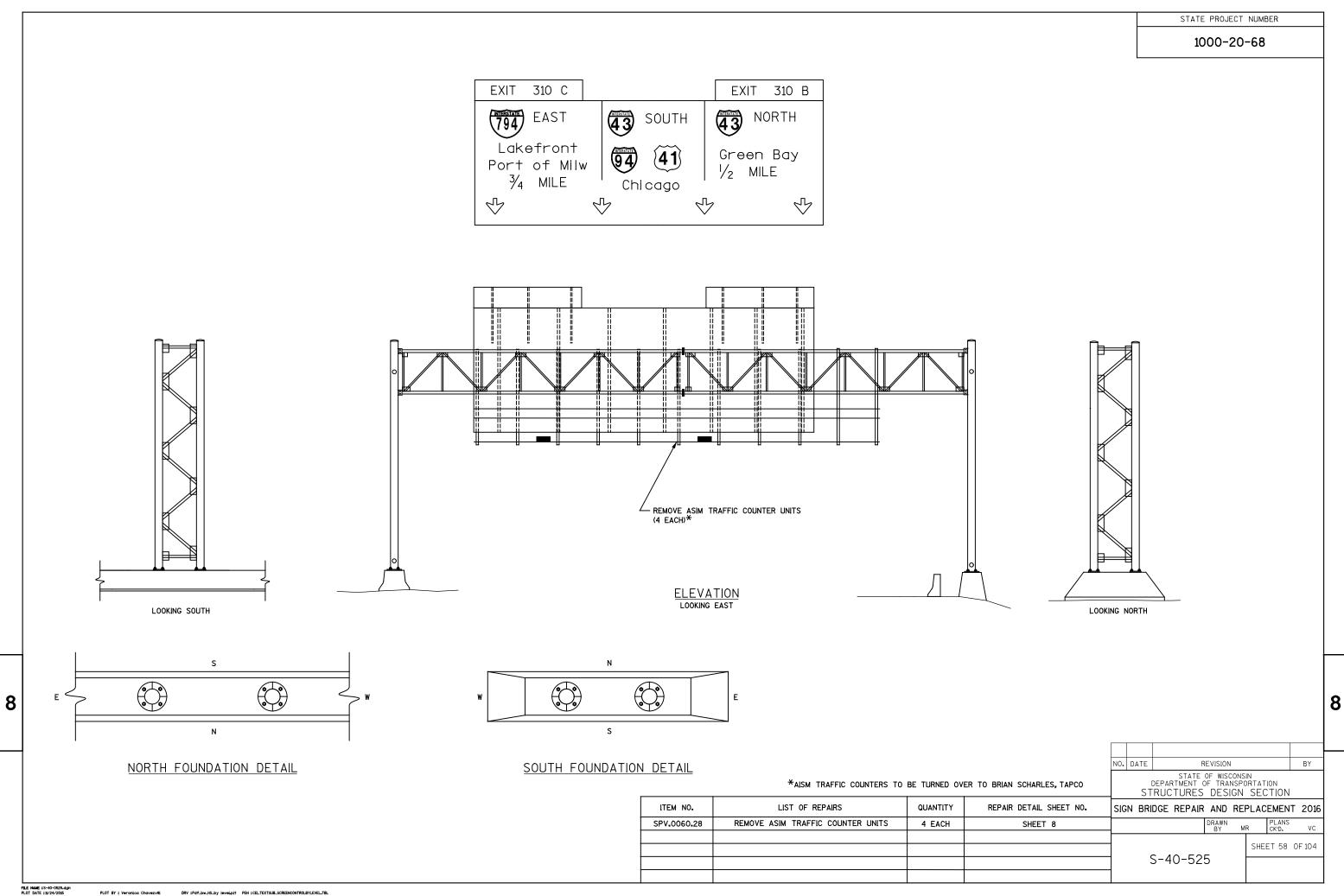
8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

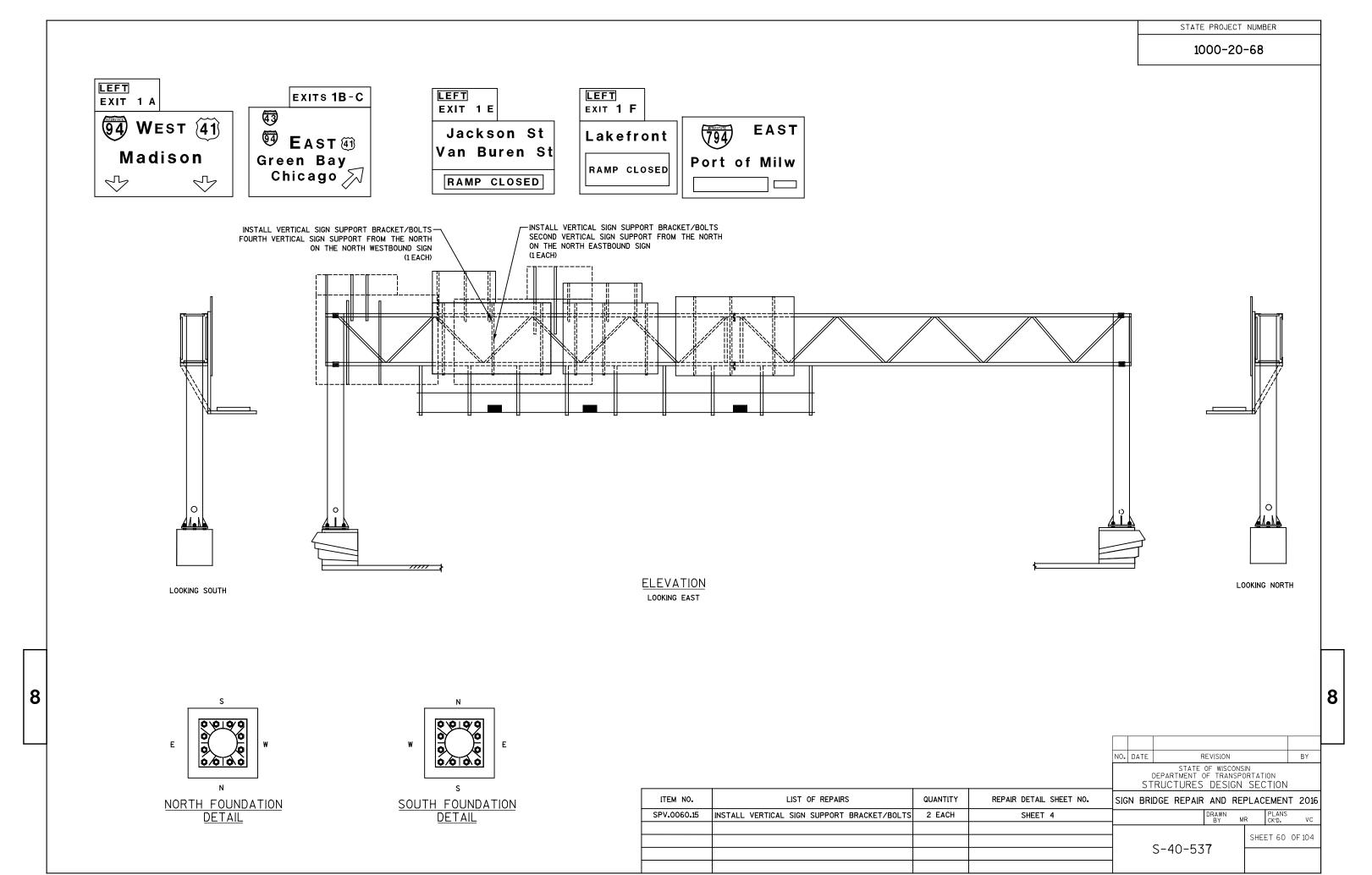
8

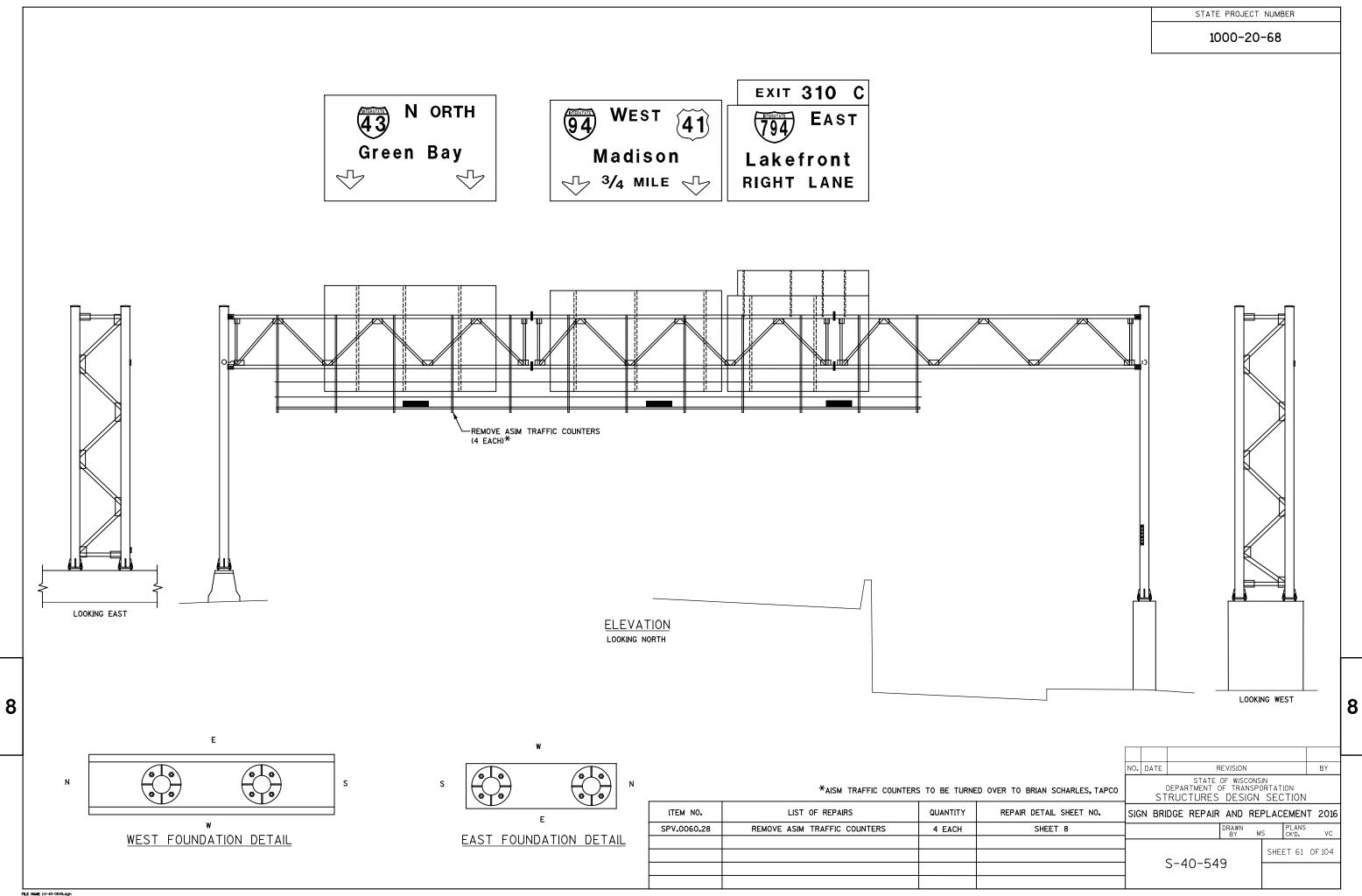
LOOKING EAST

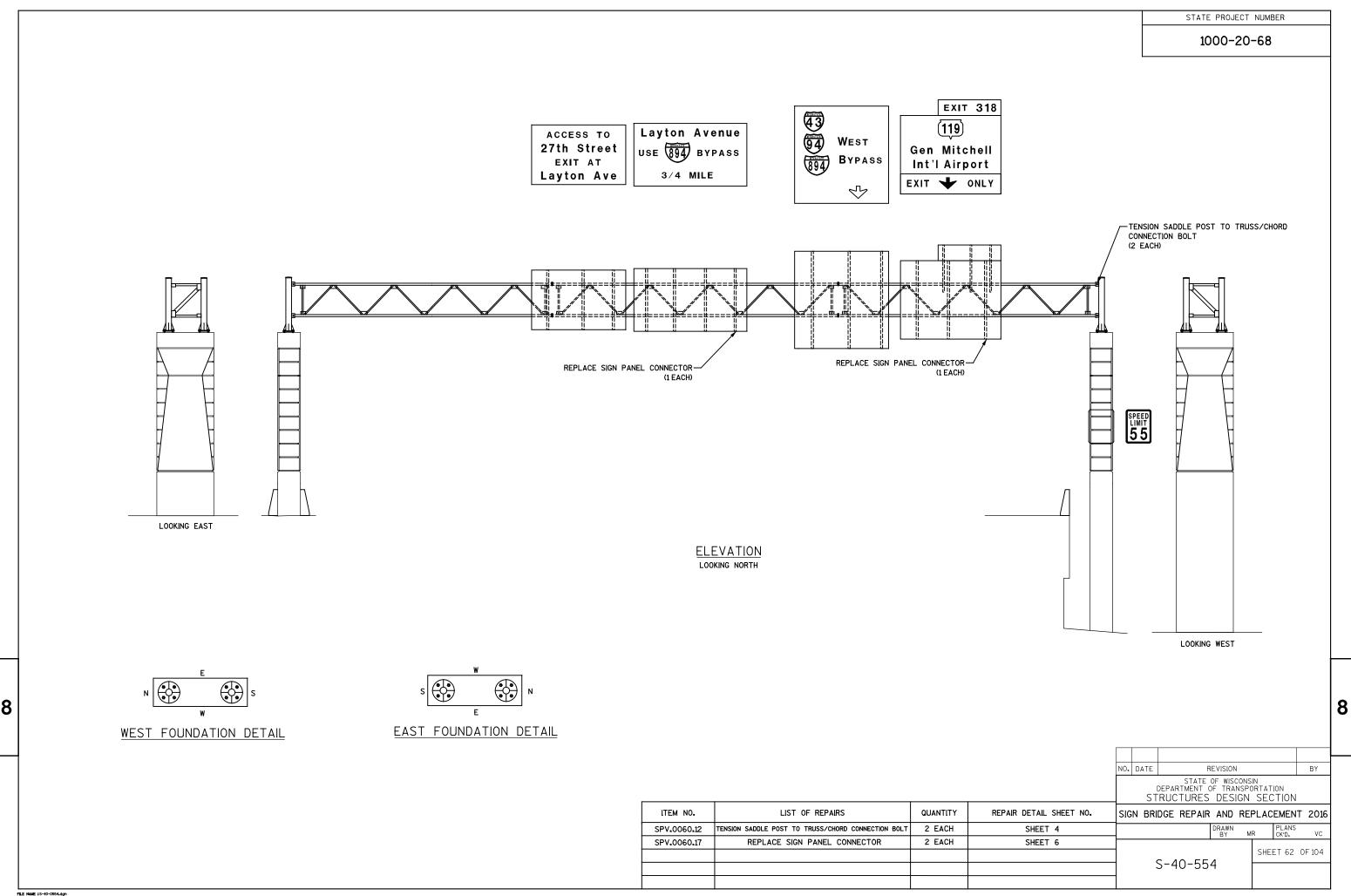
PLOT BY : Veronica Chavezv8i DRV :Pdf_bw_HS_by level.pit PEN :CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL



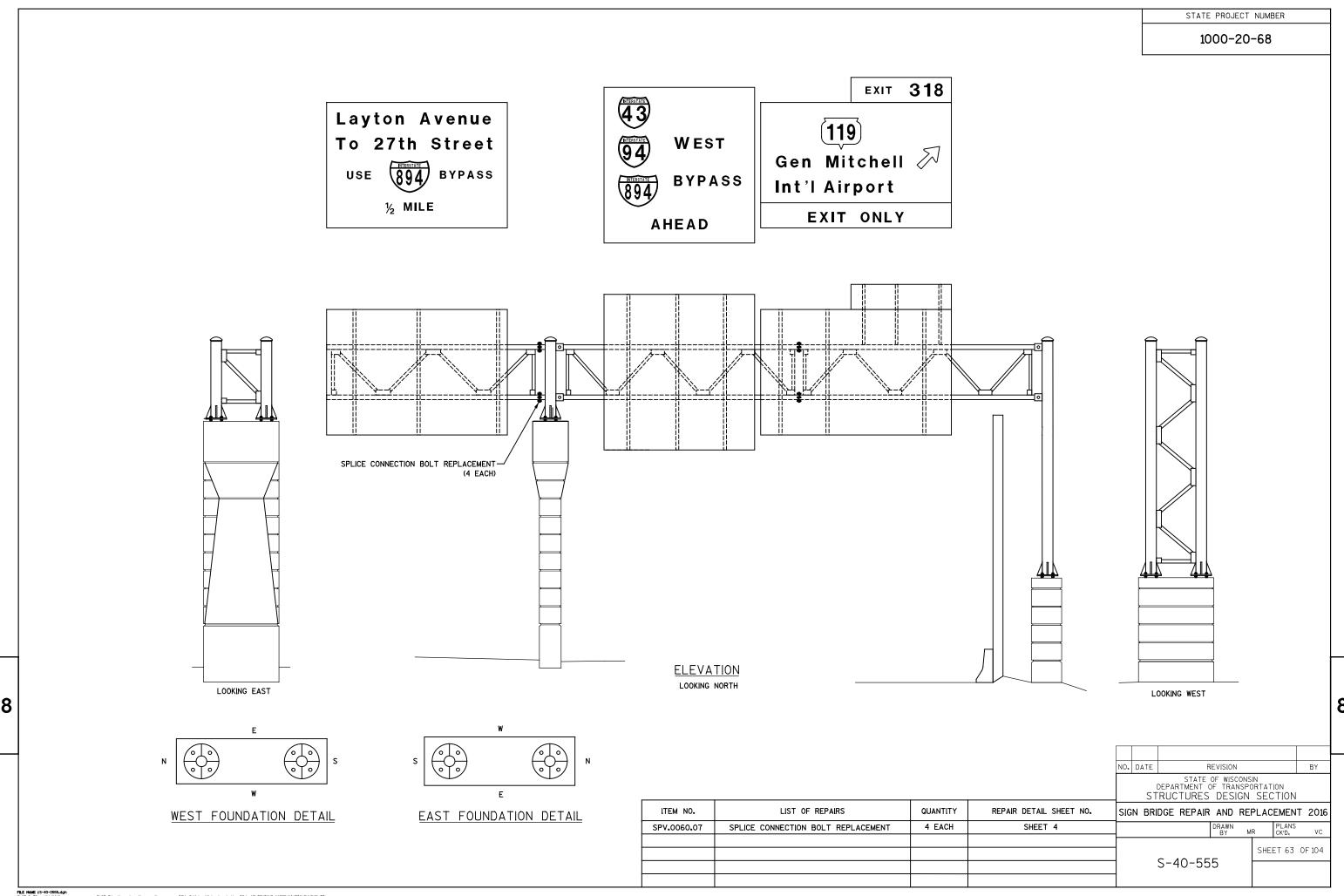
STATE PROJECT NUMBER 1000-20-68 EXIT 1 H St Paul Ave James Lovell St REPLACE TYPE II SIGN (20 SF) SUMMER-FEST PARKING **ELEVATION** 8 LOOKING EAST LOOKING NORTH NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION FOUNDATION DETAIL LIST OF REPAIRS ITEM NO. QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 REPLACE TYPE II SIGN 20 SF SHEET 7 SPV.0165.01 SHEET 59 OF 104 S-40-531

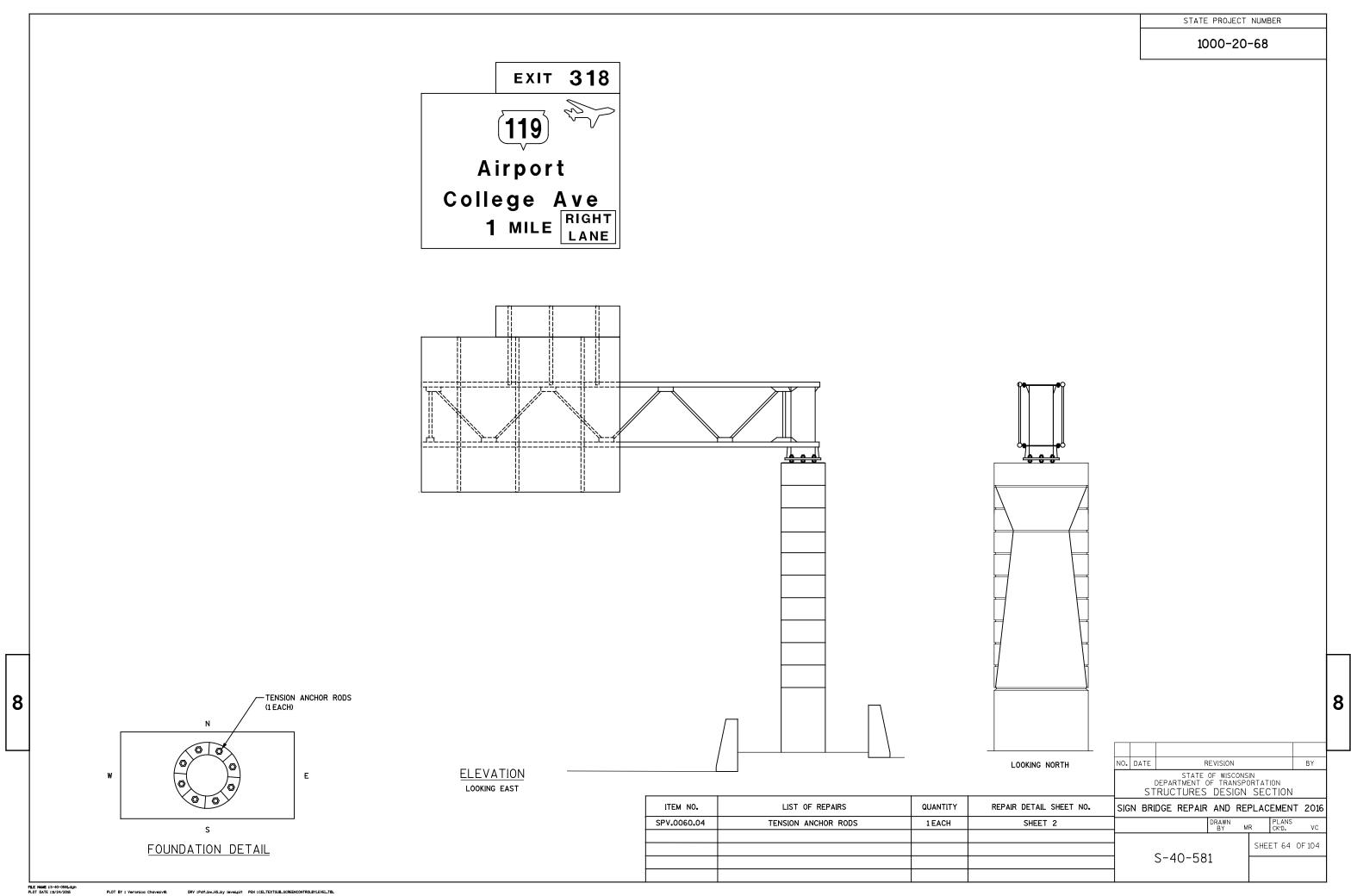


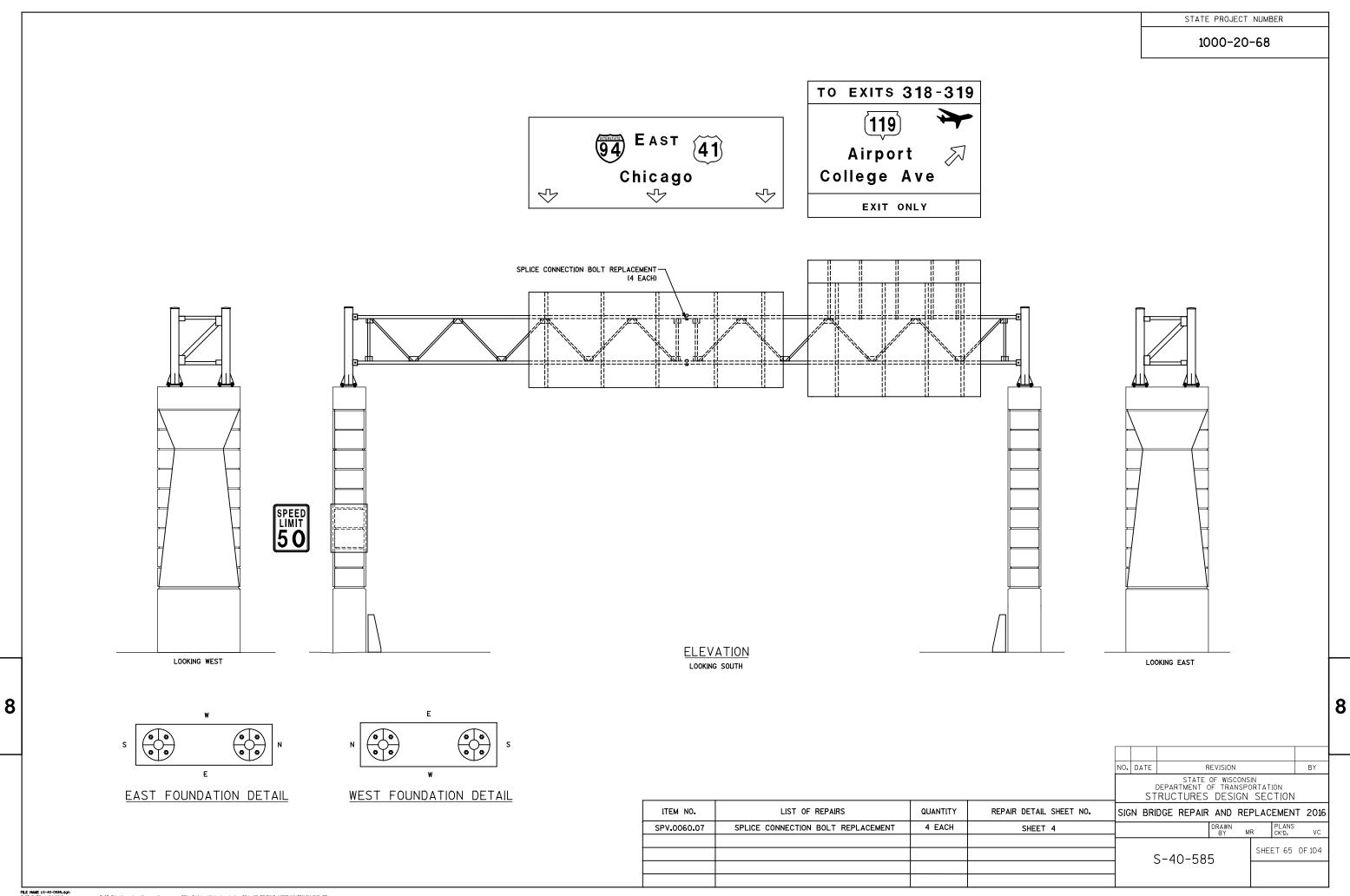


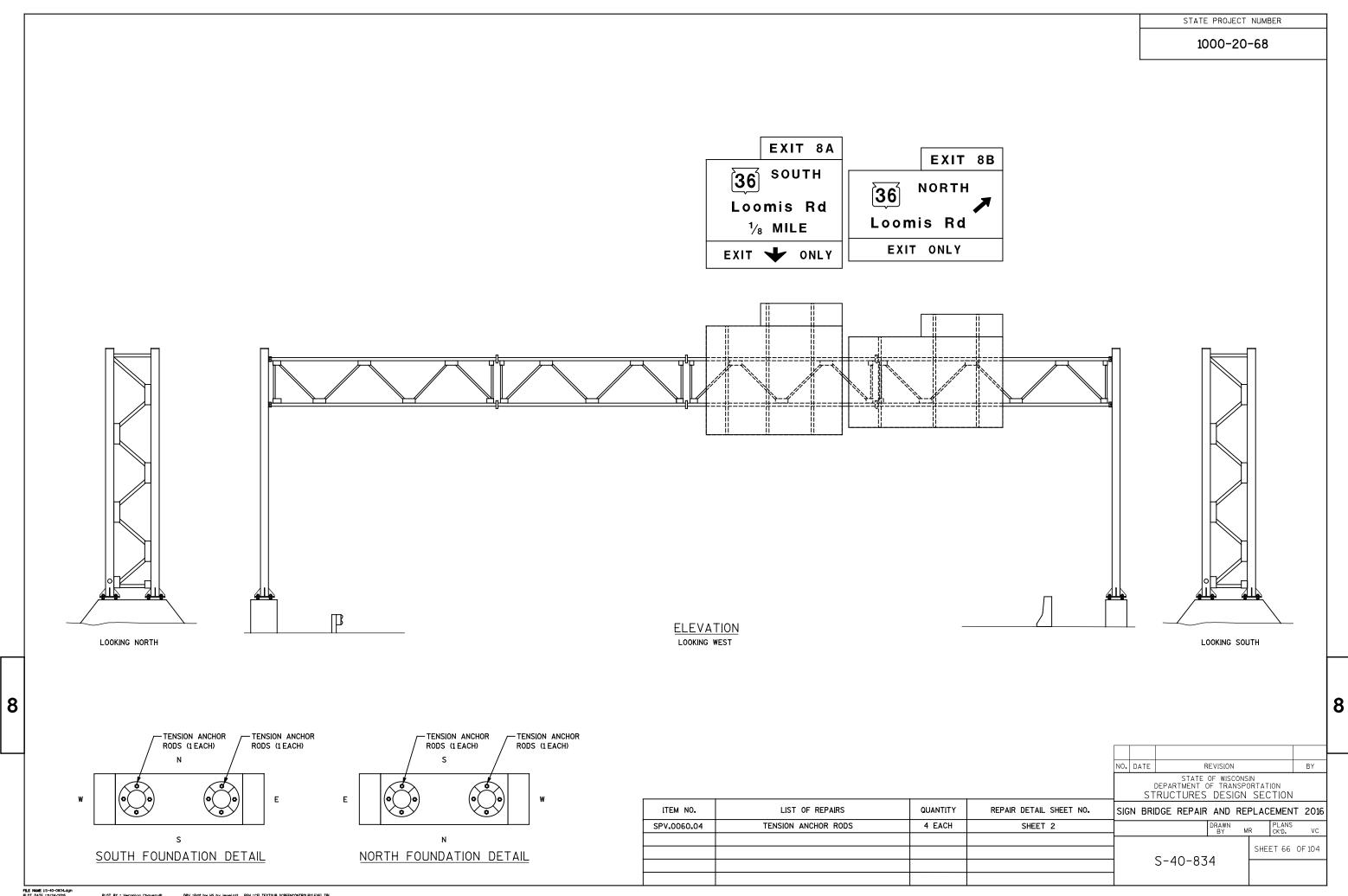


PLOT BY: Veronica Chavezv8i DRV: Pdf_bw_HS_by level.plt PEN: CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL



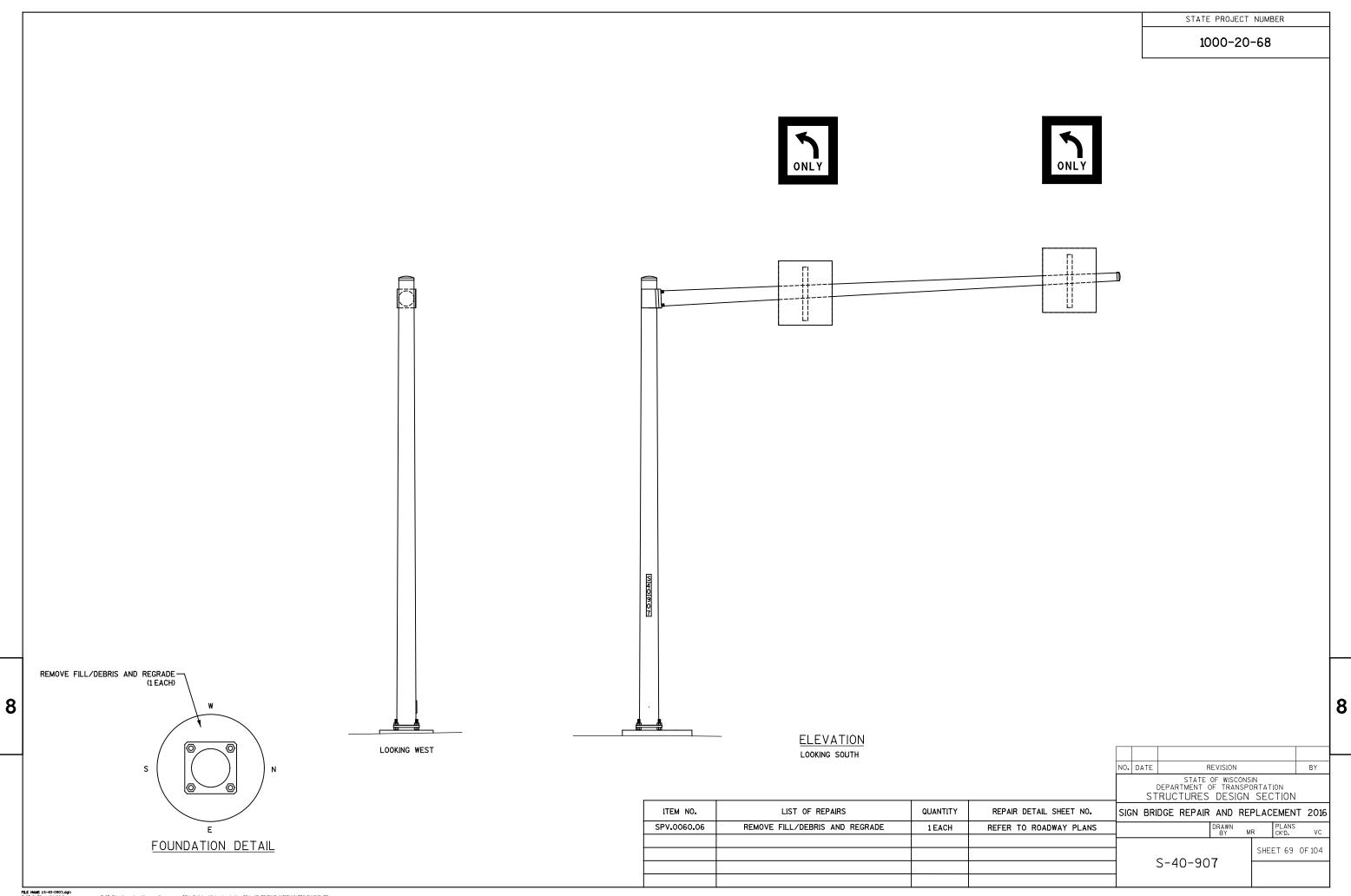


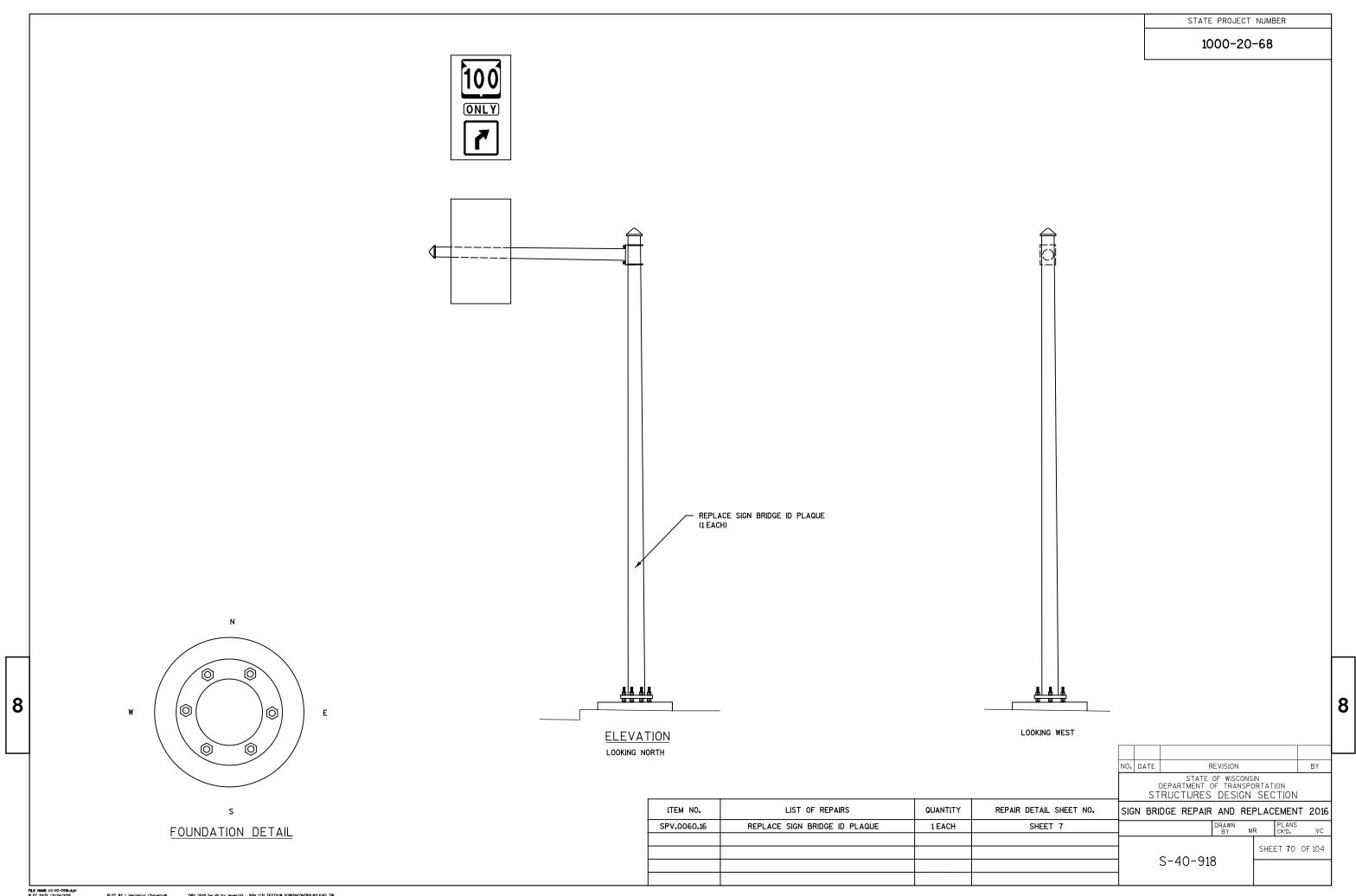


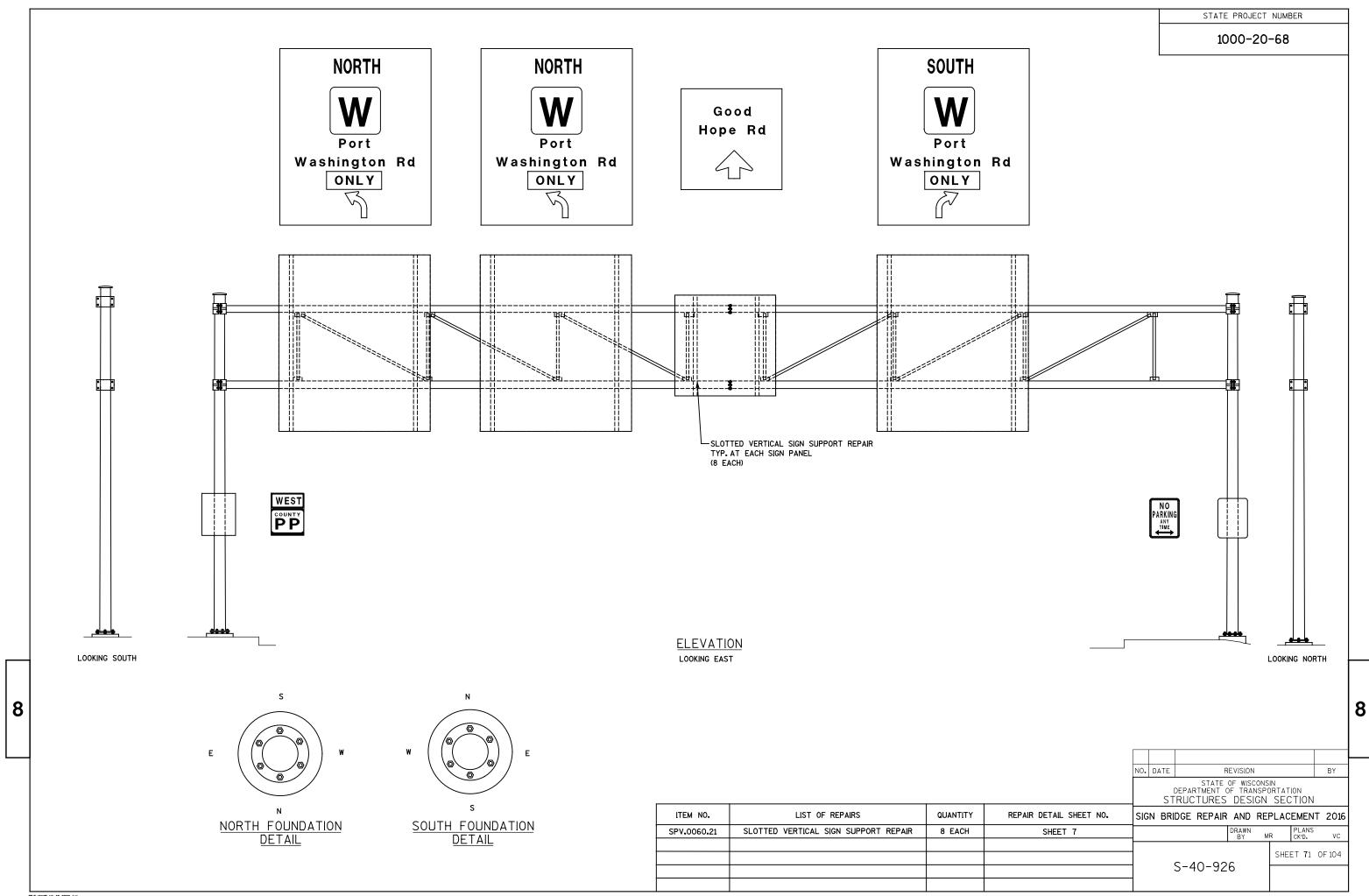


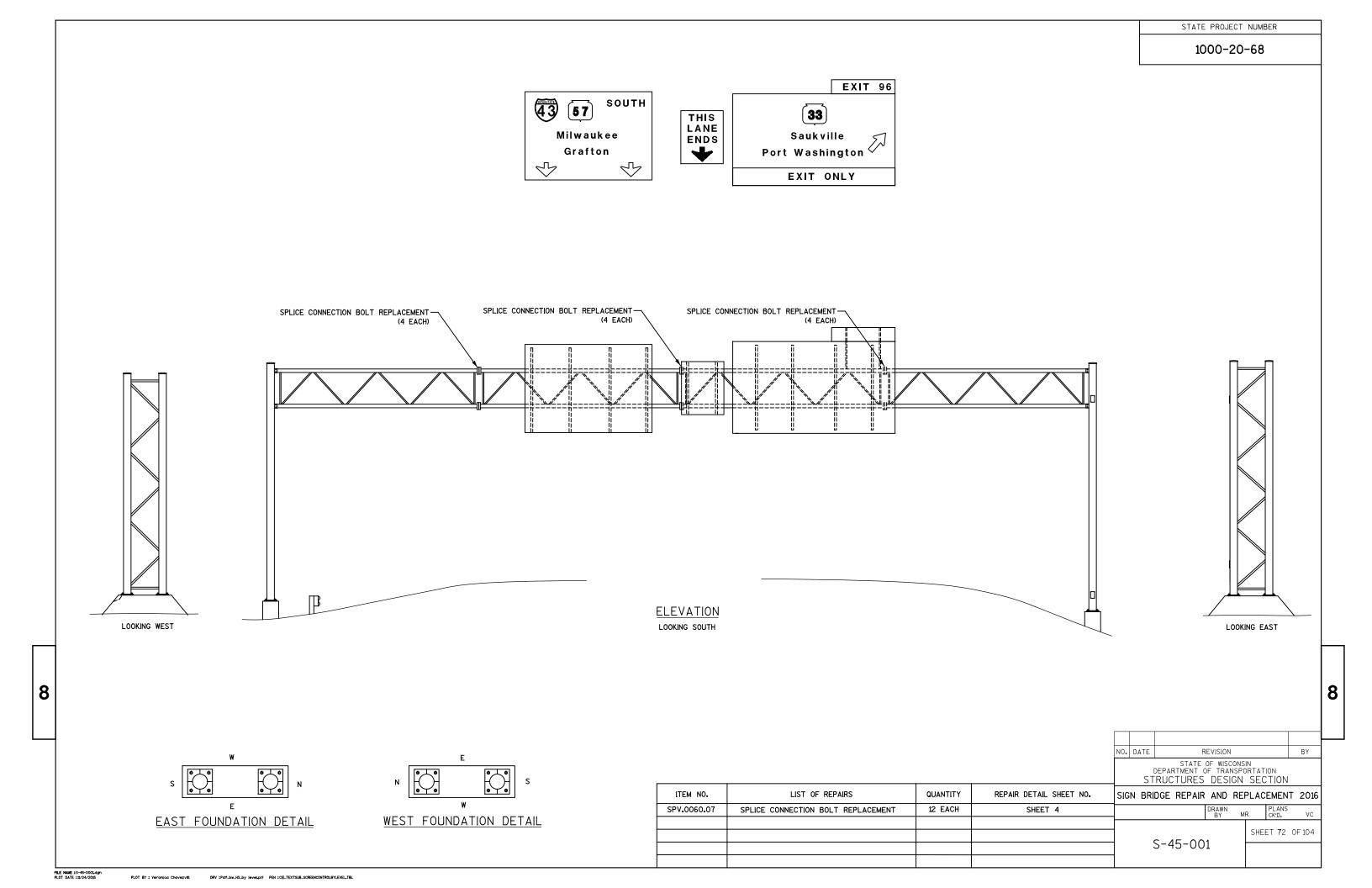
STATE PROJECT NUMBER 1000-20-68 EXIT 7 A **FREEWAY** LANE Grantosa Dr **ENDS ENDS** Villard Ave /₄ MILE $\frac{1}{2}$ MILE **EXIT ONLY** -REPLACE SIGN PANEL CONNECTOR (1 EACH) SECURE HANDHOLE COVER LOOKING EAST **ELEVATION** LOOKING SOUTH LOOKING WEST TENSION ANCHOR RODS-8 8 (1 EACH) REMOVE FILL/DEBRIS AND REGRADE-(1 EACH) NO. DATE S S STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 1 EACH SHEET 2 TENSION ANCHOR RODS EAST FOUNDATION DETAIL WEST FOUNDATION DETAIL 1 EACH SPV.0060.06 REMOVE FILL/DEBRIS AND REGRADE REFER TO ROADWAY PLANS SHEET 67 OF 104 SPV.0060.17 REPLACE SIGN PANEL CONNECTOR 1 EACH SHEET 6 S-40-844 1 EACH SPV.0060.23 SECURE HANDHOLE COVER SHEET 8 PLOT BY : Veronica Chavezv8l DRV :Pdf_bw_HS_by level.plt PEN :CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL

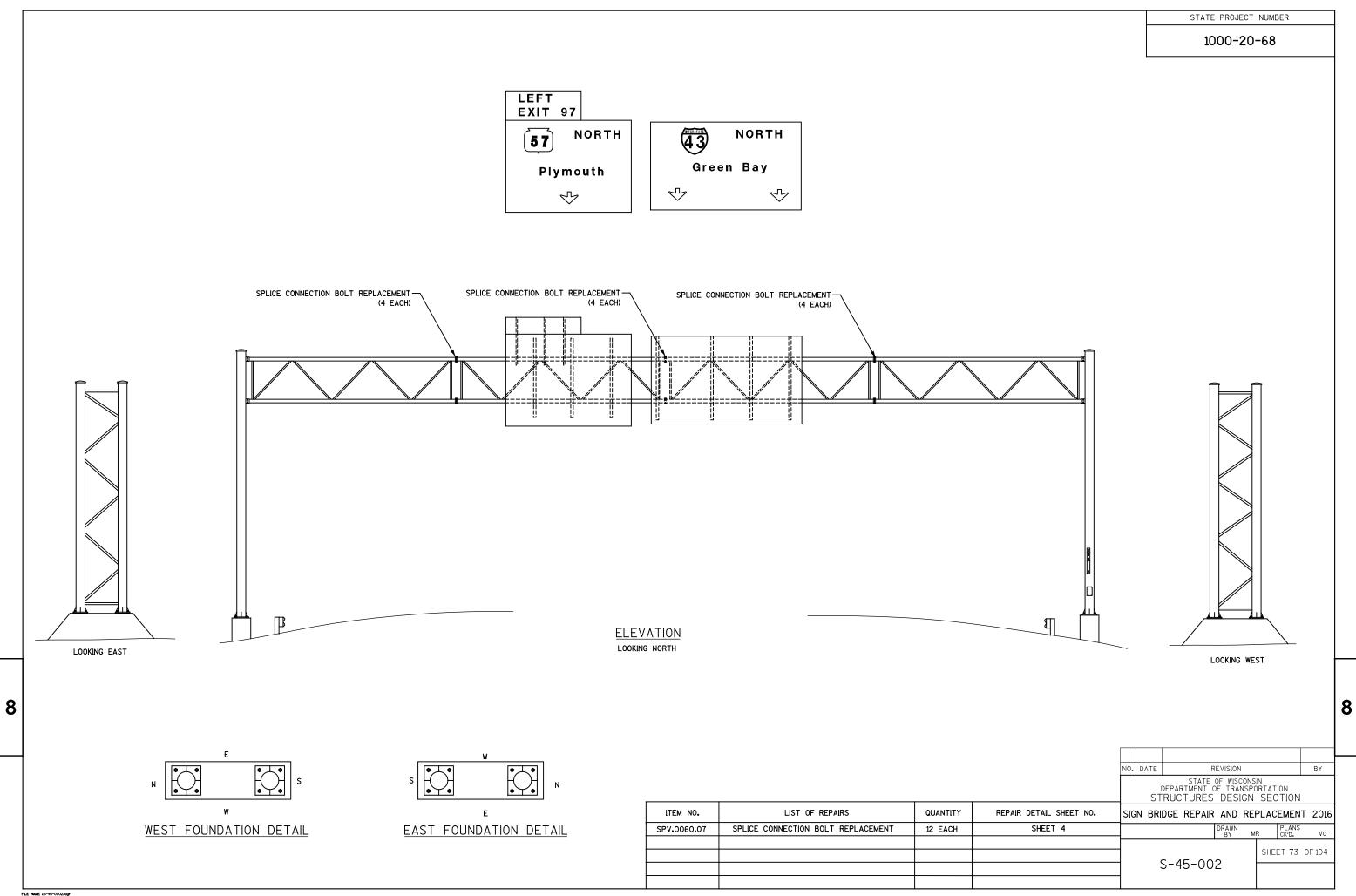
STATE PROJECT NUMBER 1000-20-68 -TIGHTEN SIGNAL MOUNT SOCKET CONNECTION (2 EACH) WORK **ELEVATION** LOOKING EAST LOOKING NORTH 8 8 -TENSION ANCHOR RODS NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 1 EACH SHEET 2 SPV.0060.26 2 EACH SHEET 8 TIGHTEN SIGNAL MOUNT SOCKET CONNECTION FOUNDATION DETAIL SHEET 68 OF 104 S-40-903



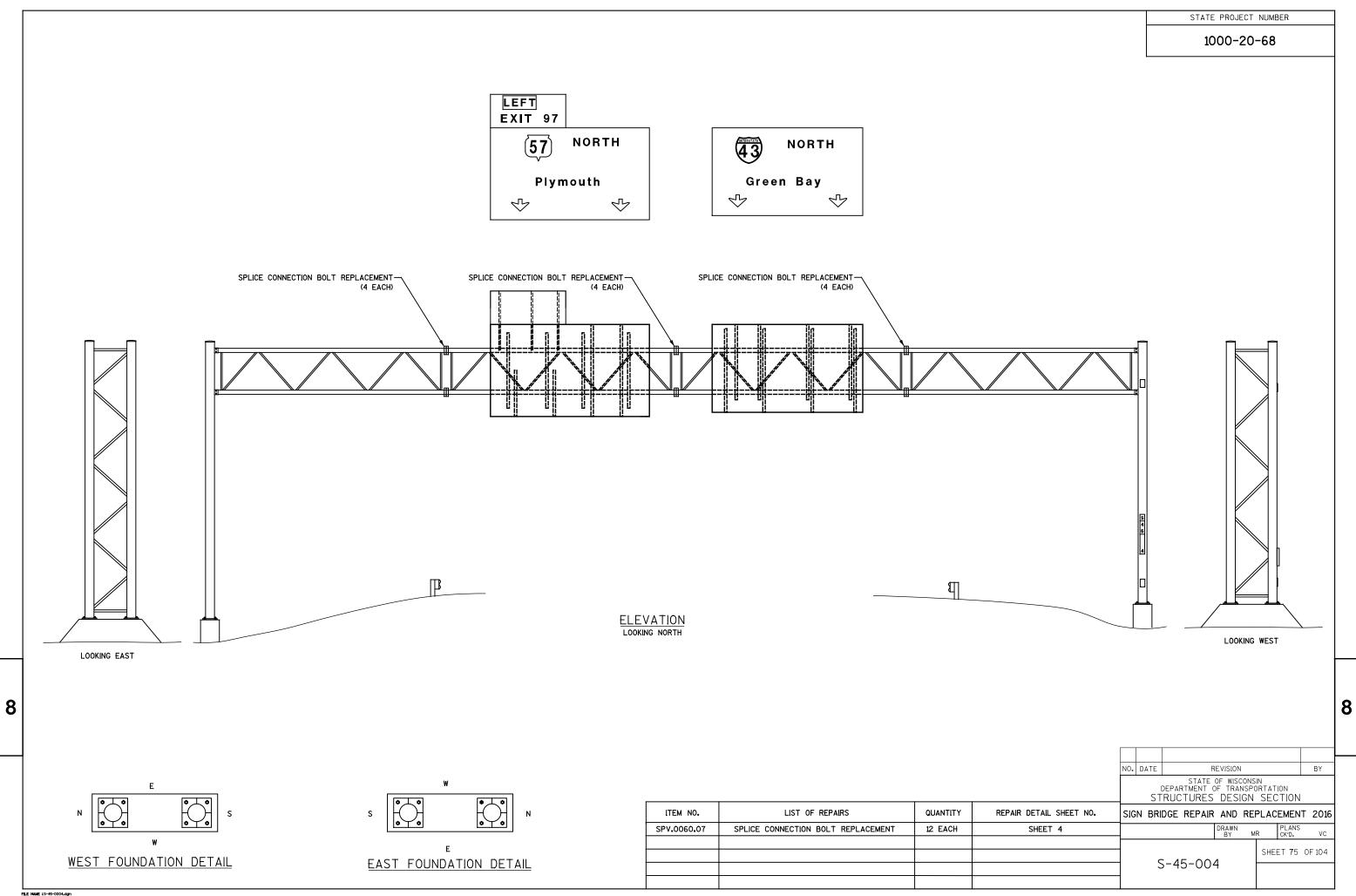


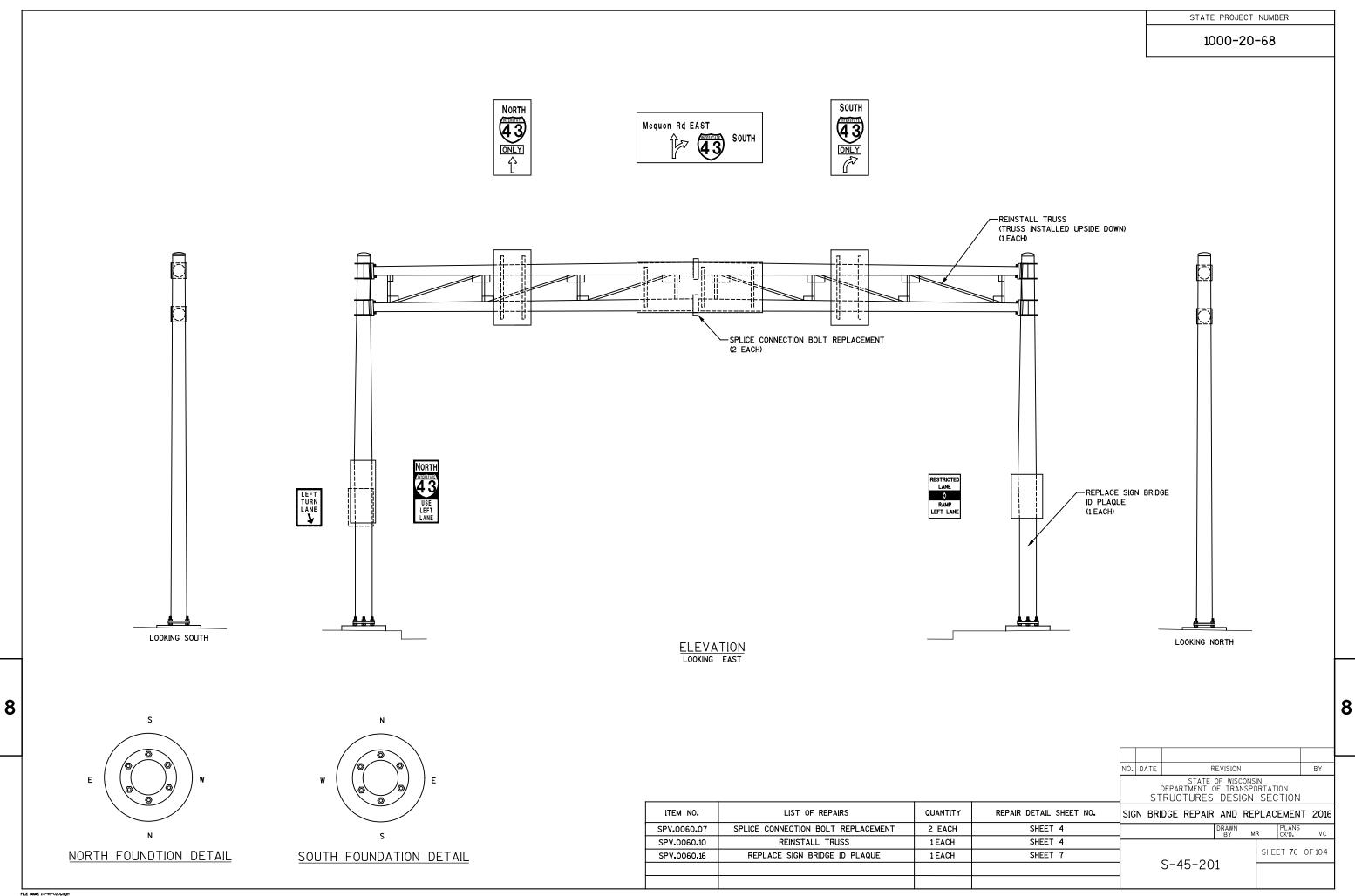




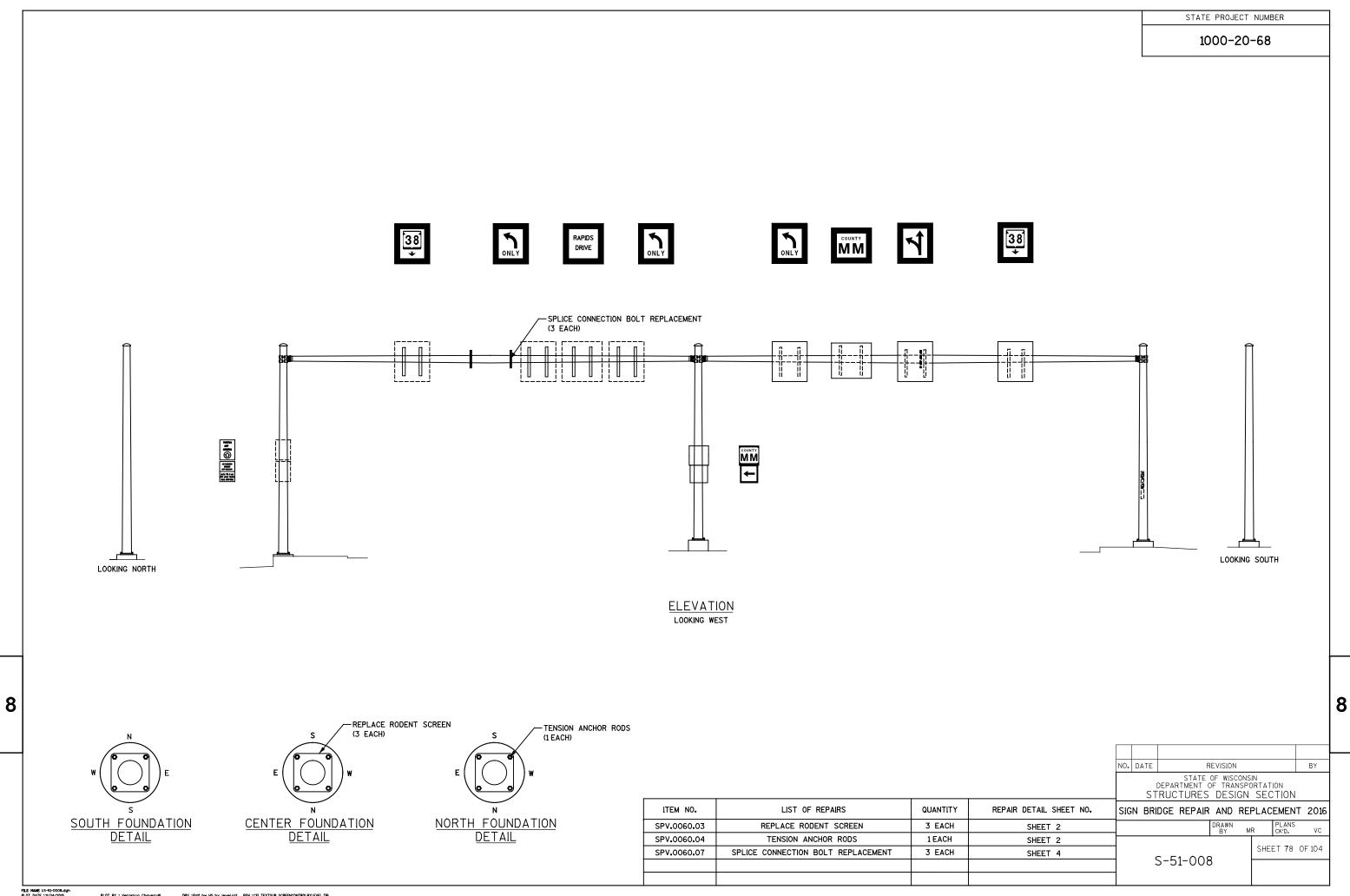


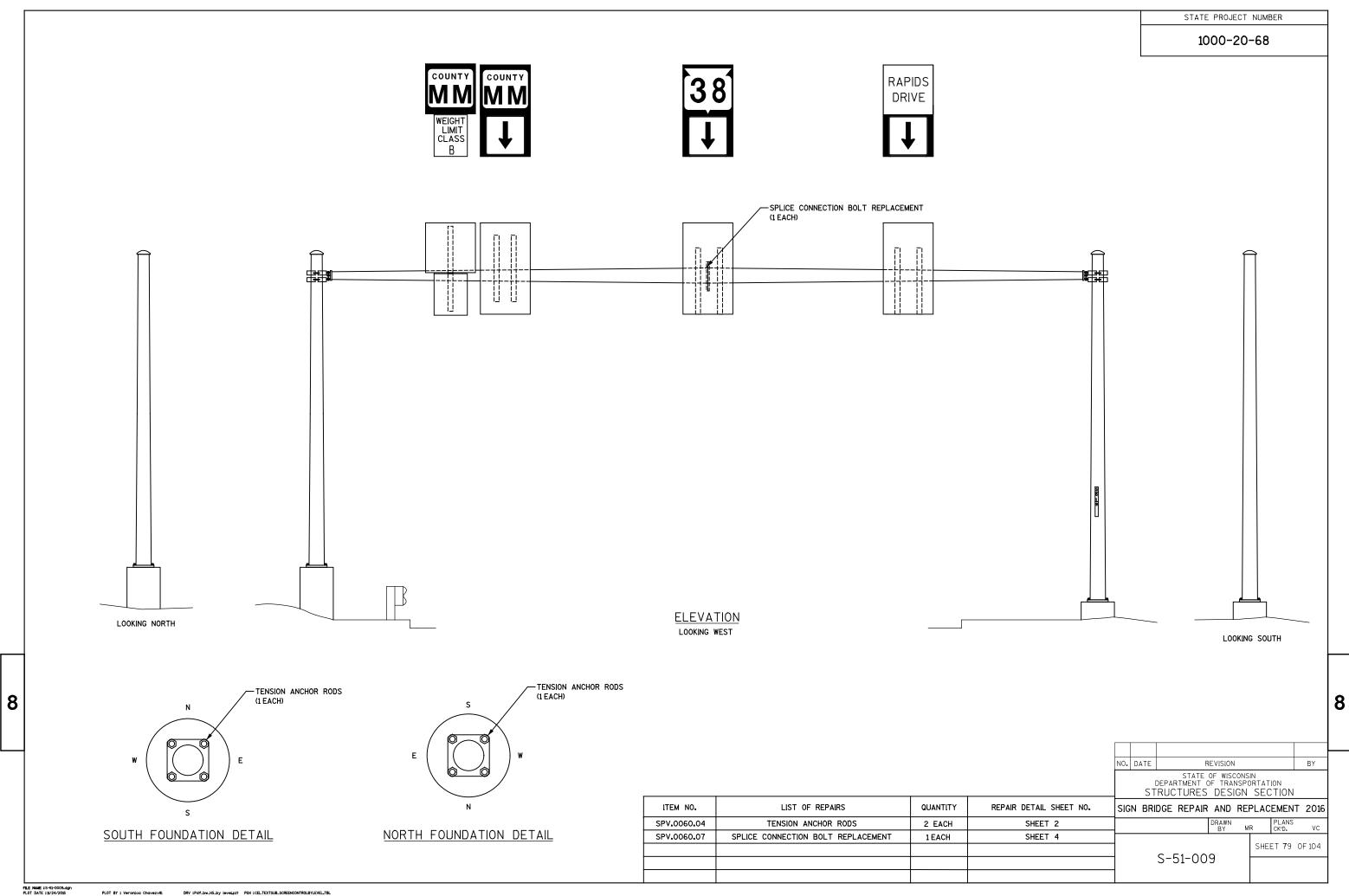
STATE PROJECT NUMBER 1000-20-68 EXIT 96 SOUTH **(57) 33** THIS LANE ENDS Saukville Milwaukee Port Washington Grafton EXIT 🗣 ONLY SPLICE CONNECTION BOLT REPLACEMENT(4 EACH) SPLICE CONNECTION BOLT REPLACEMENT SPLICE CONNECTION BOLT REPLACEMENT-(4 EACH) (4 EACH) **ELEVATION** LOOKING EAST LOOKING SOUTH LOOKING WEST 8 8 Ε NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION EAST FOUNDATION DETAIL WEST FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.07 SPLICE CONNECTION BOLT REPLACEMENT 12 EACH SHEET 4 SHEET 74 OF 104 S-45-003 PLOT BY : Veronica Chavezv8l DRV :Pdf_bw_HS_by level.plt PEN :CEL_TEXTSUB_SCREENCONTROLBYLEVEL_TBL

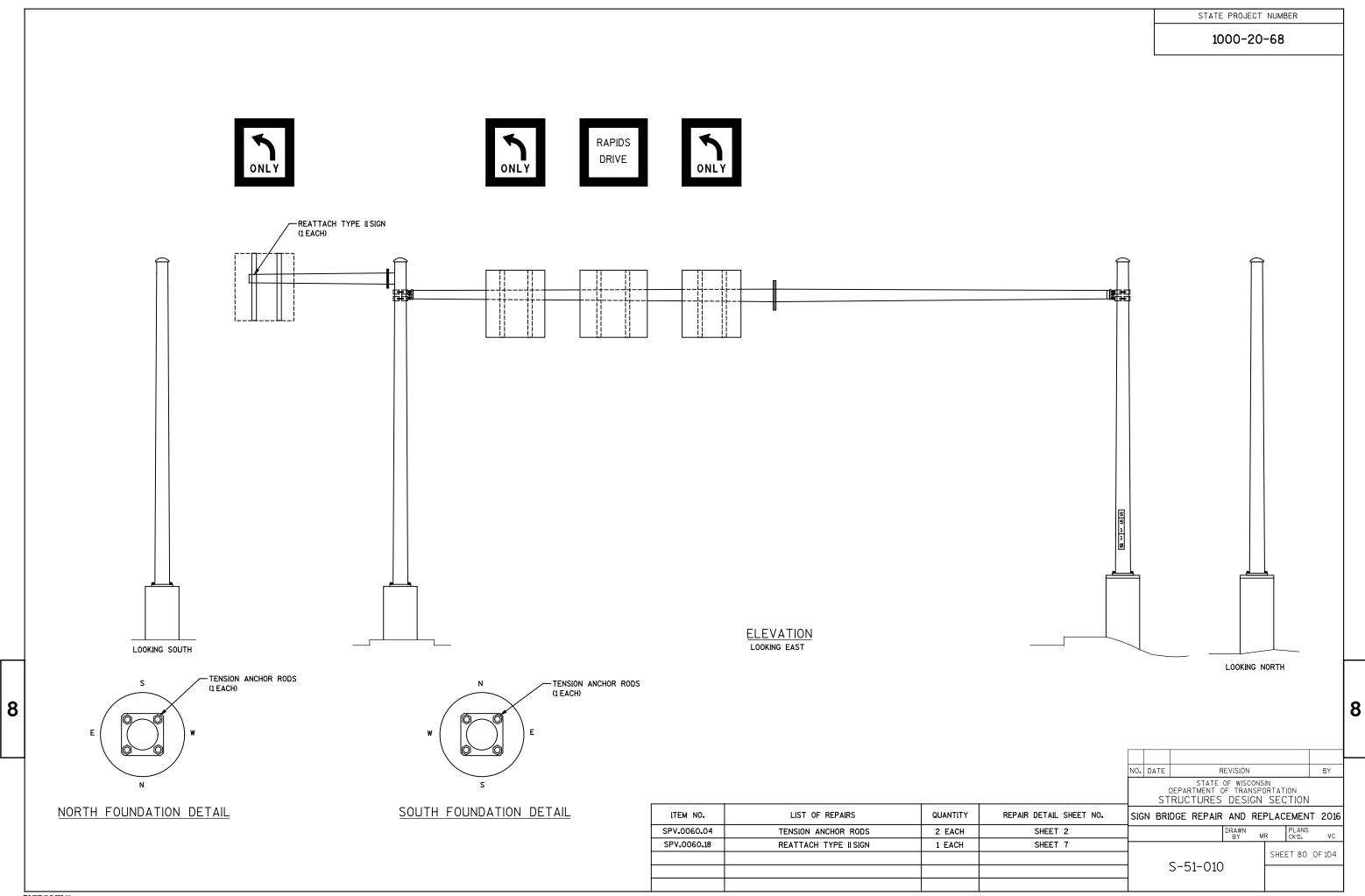


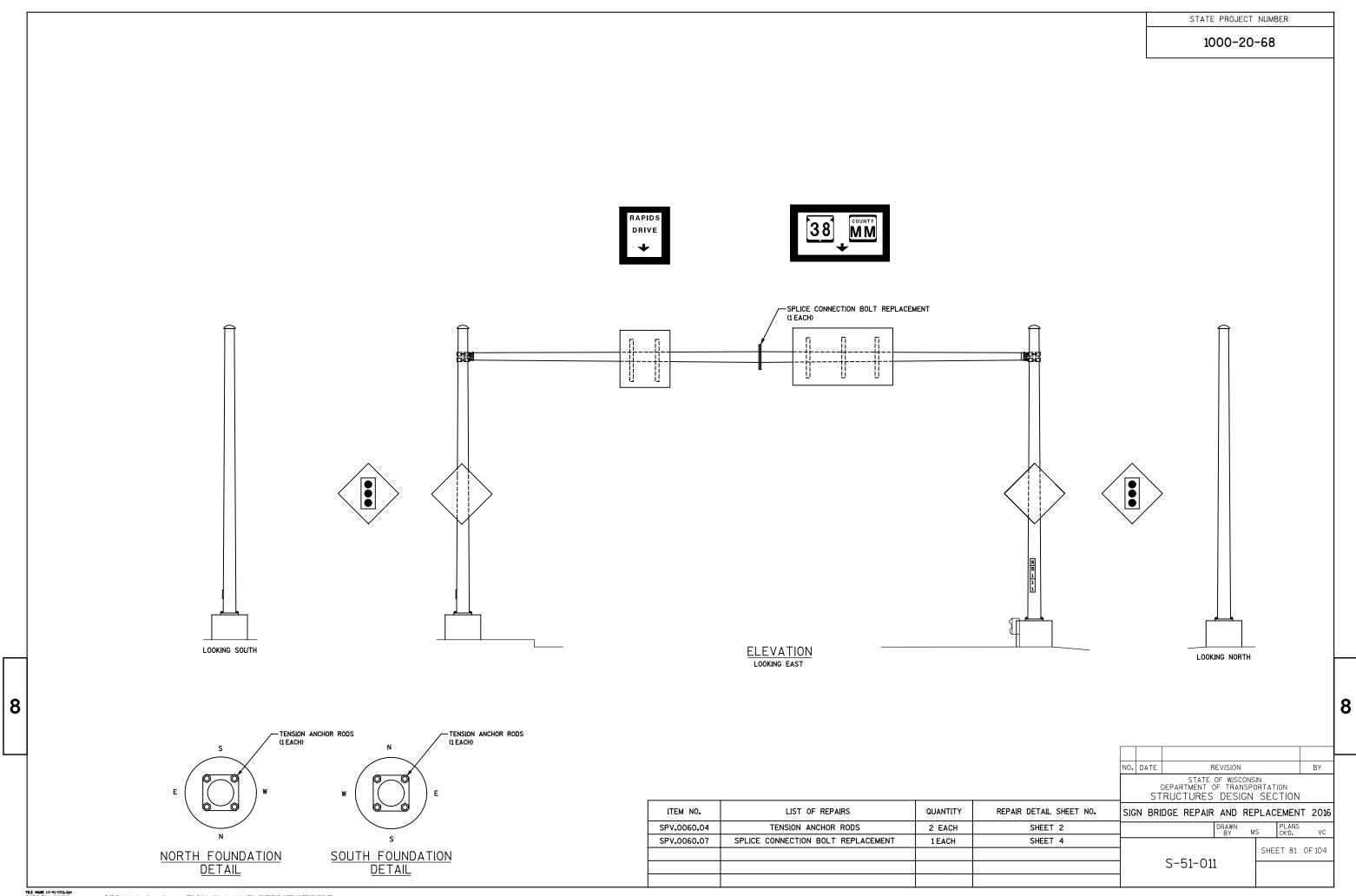


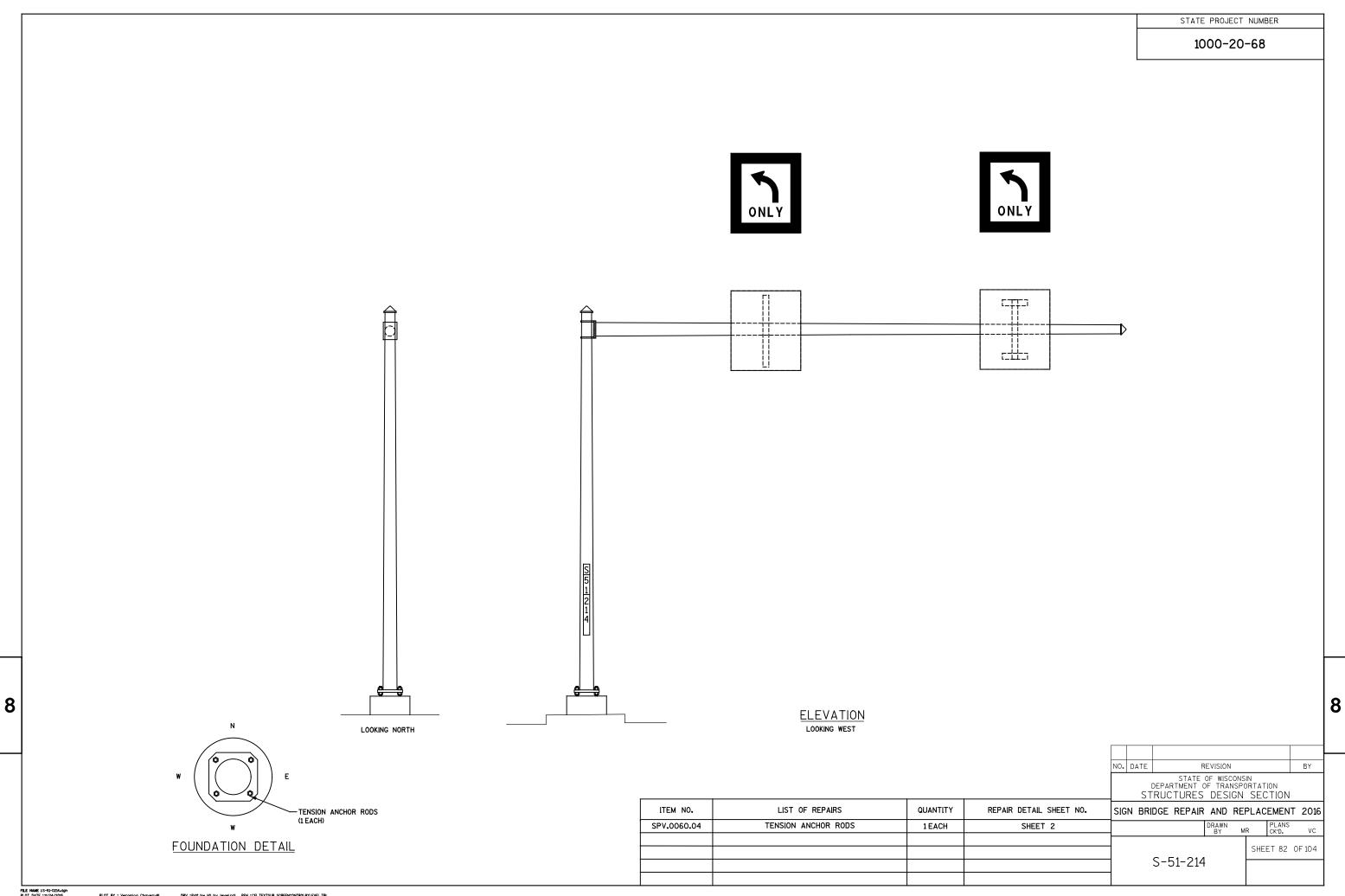
STATE PROJECT NUMBER 1000-20-68 TO SOUTH ONLY Mequon Rd EAST - REPLACE SIGN BRIDGE ID PLAQUE S LOOKING WEST 8 8 **ELEVATION** LOOKING SOUTH NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION FOUNDATION DETAIL ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.16 REPLACE SIGN BRIDGE ID PLAQUE 1 EACH SHEET 7 SHEET 77 OF 104 S-45-600 FILE NAME : S-45-0600.dgn PLOT DATE : 11/24/2015 PLOT BY: Veronica Chavezv8l DRV: Pdf_bw_HS_by level.plt PEN: CEL_TEXTSUB_SCREENCONTROLBYLEVEL_TBL

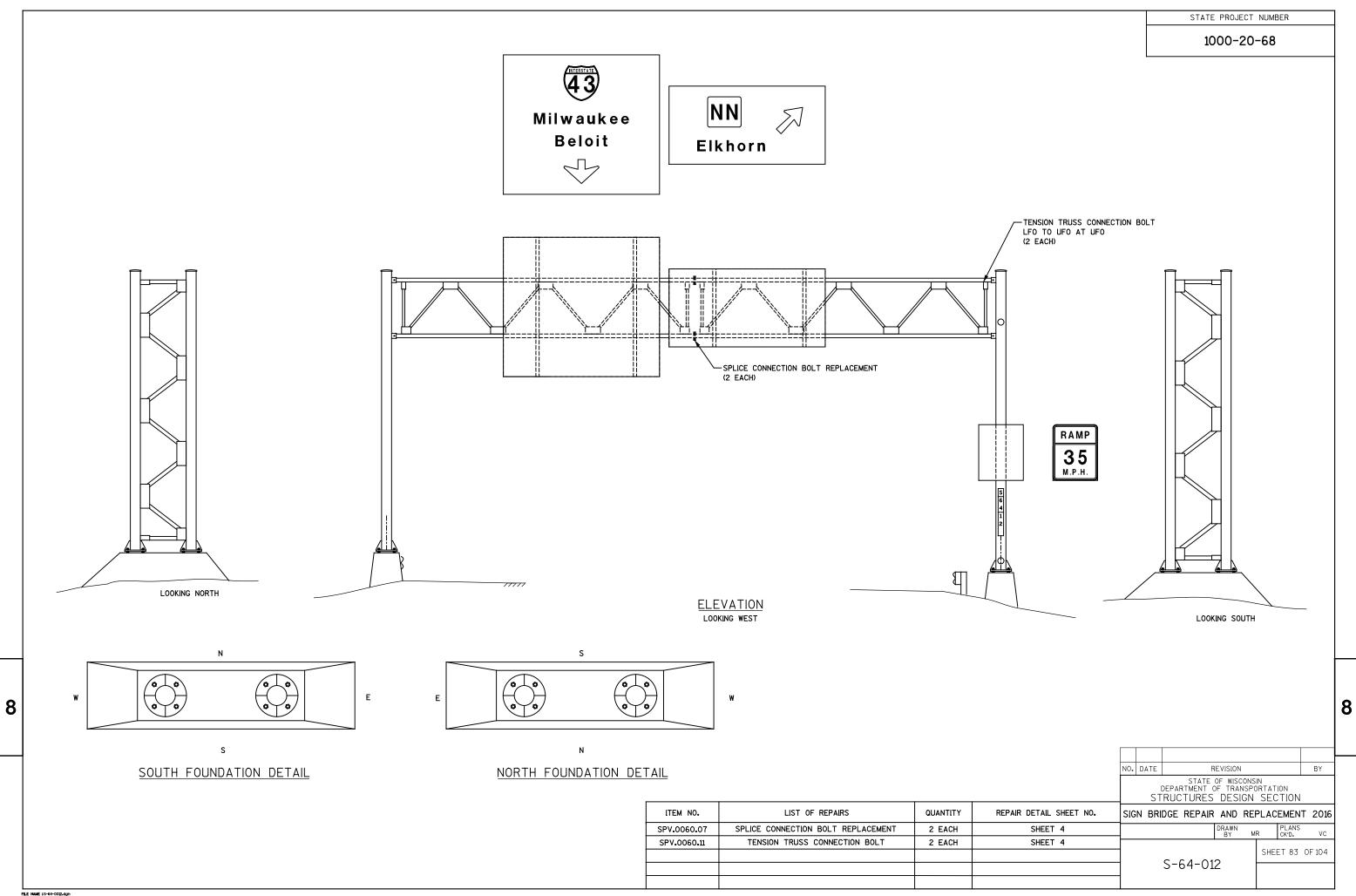


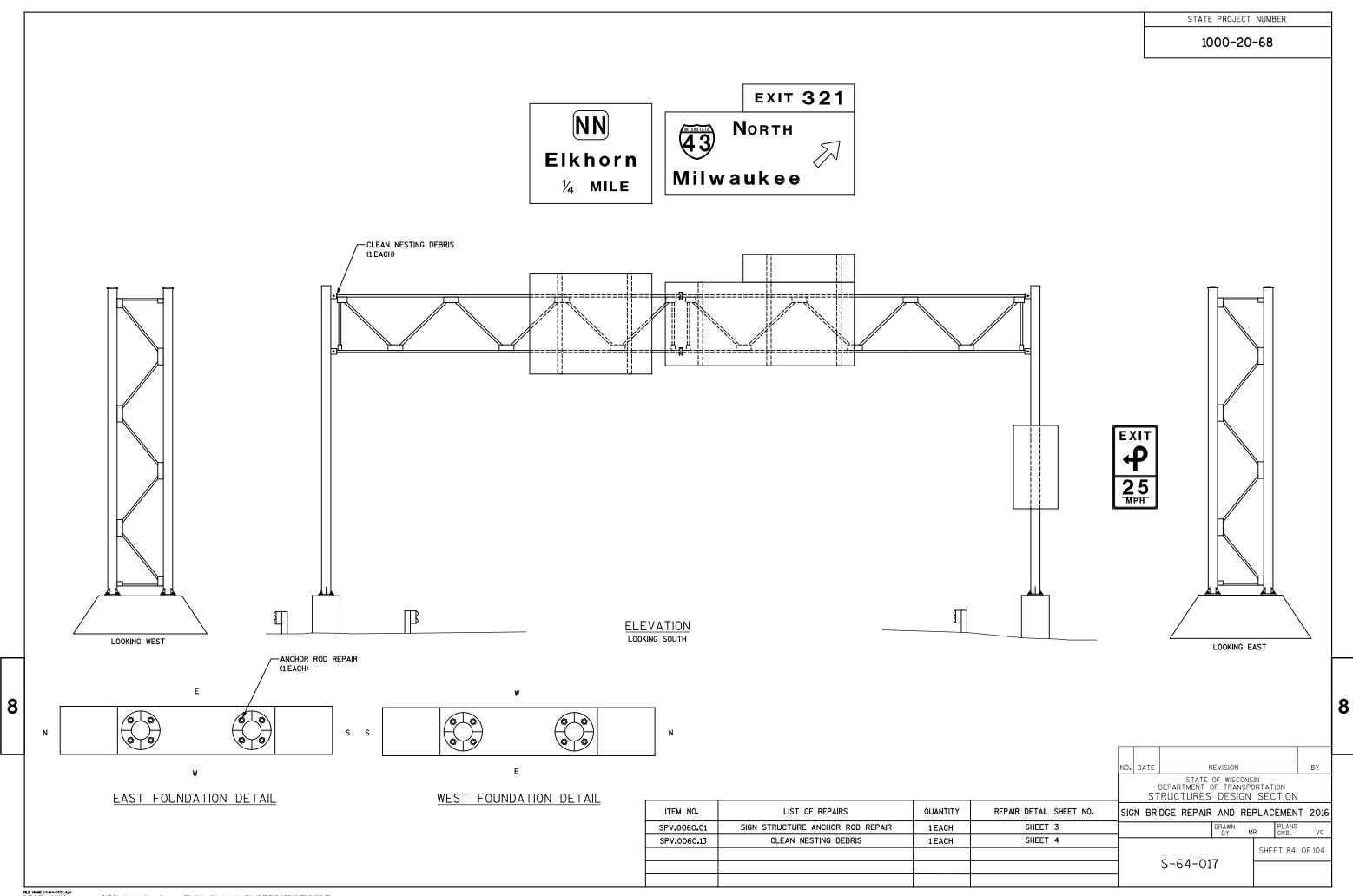






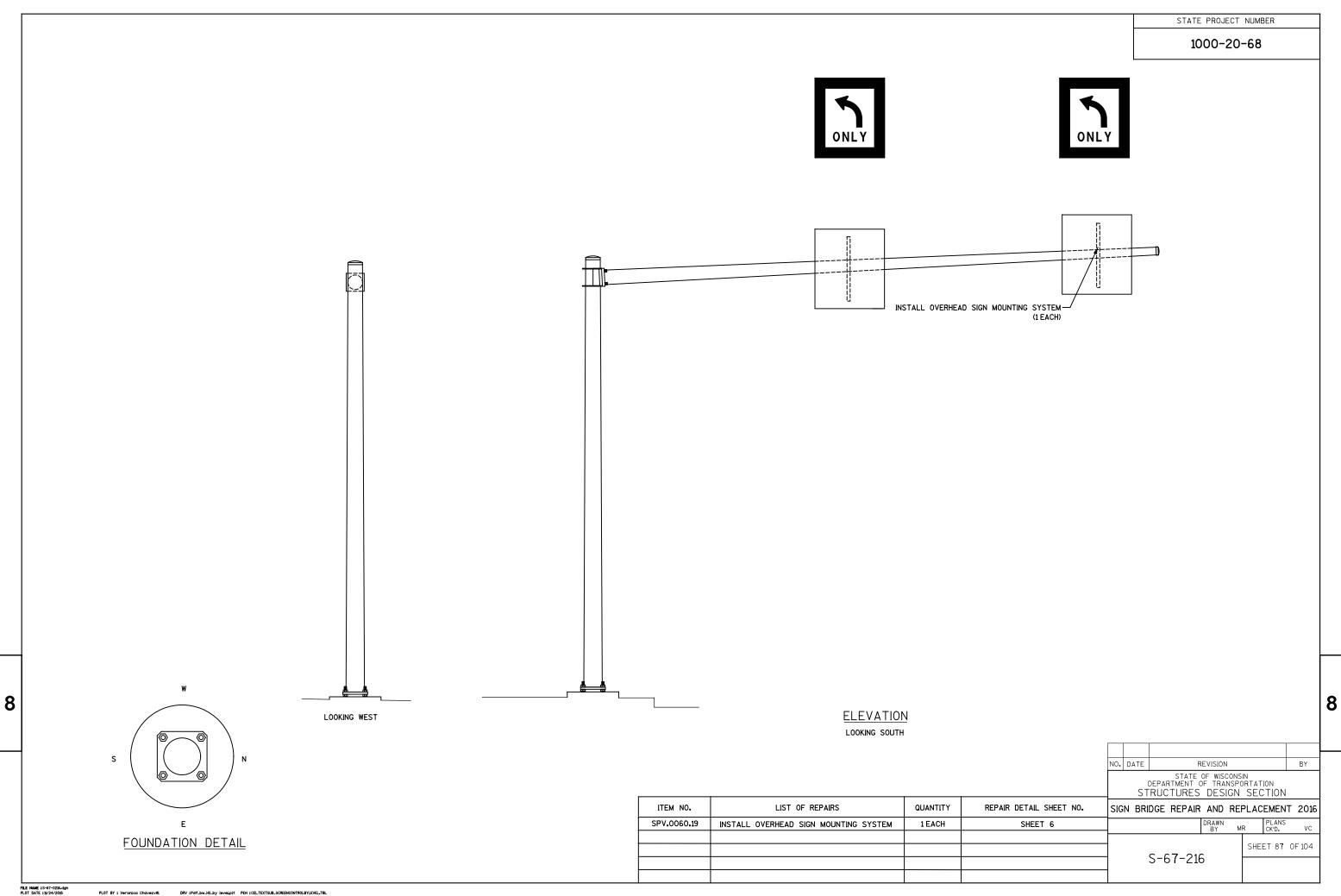


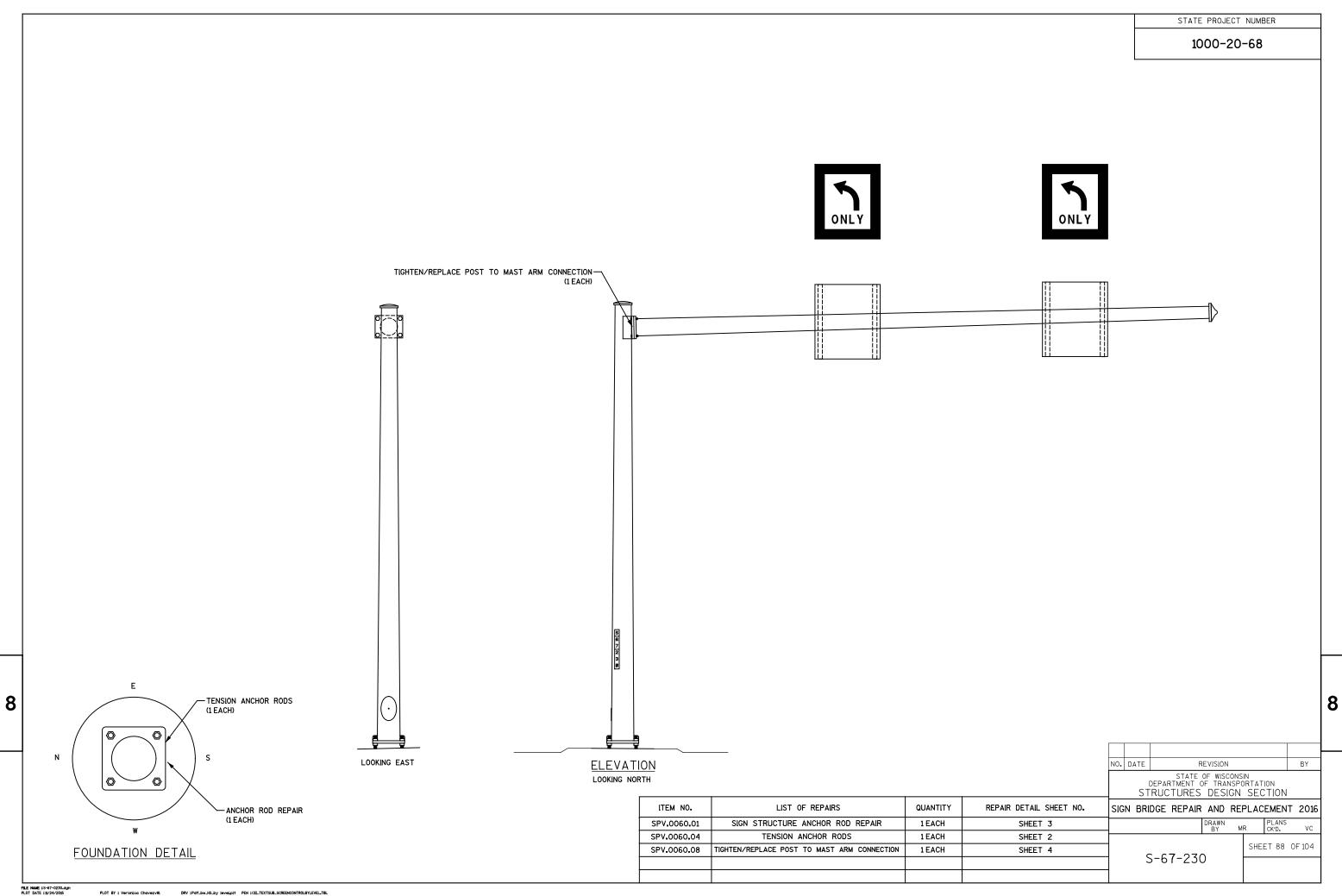


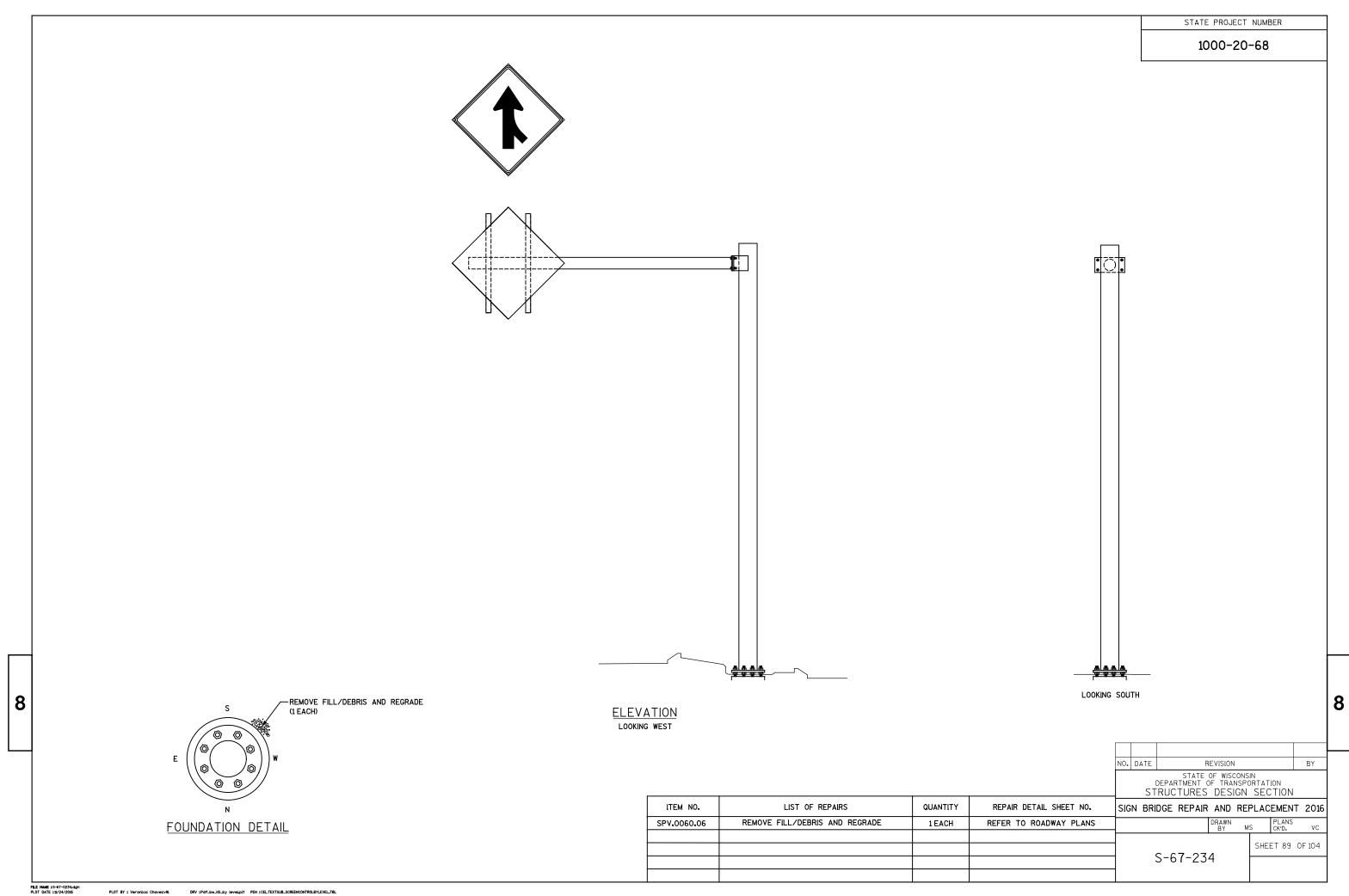


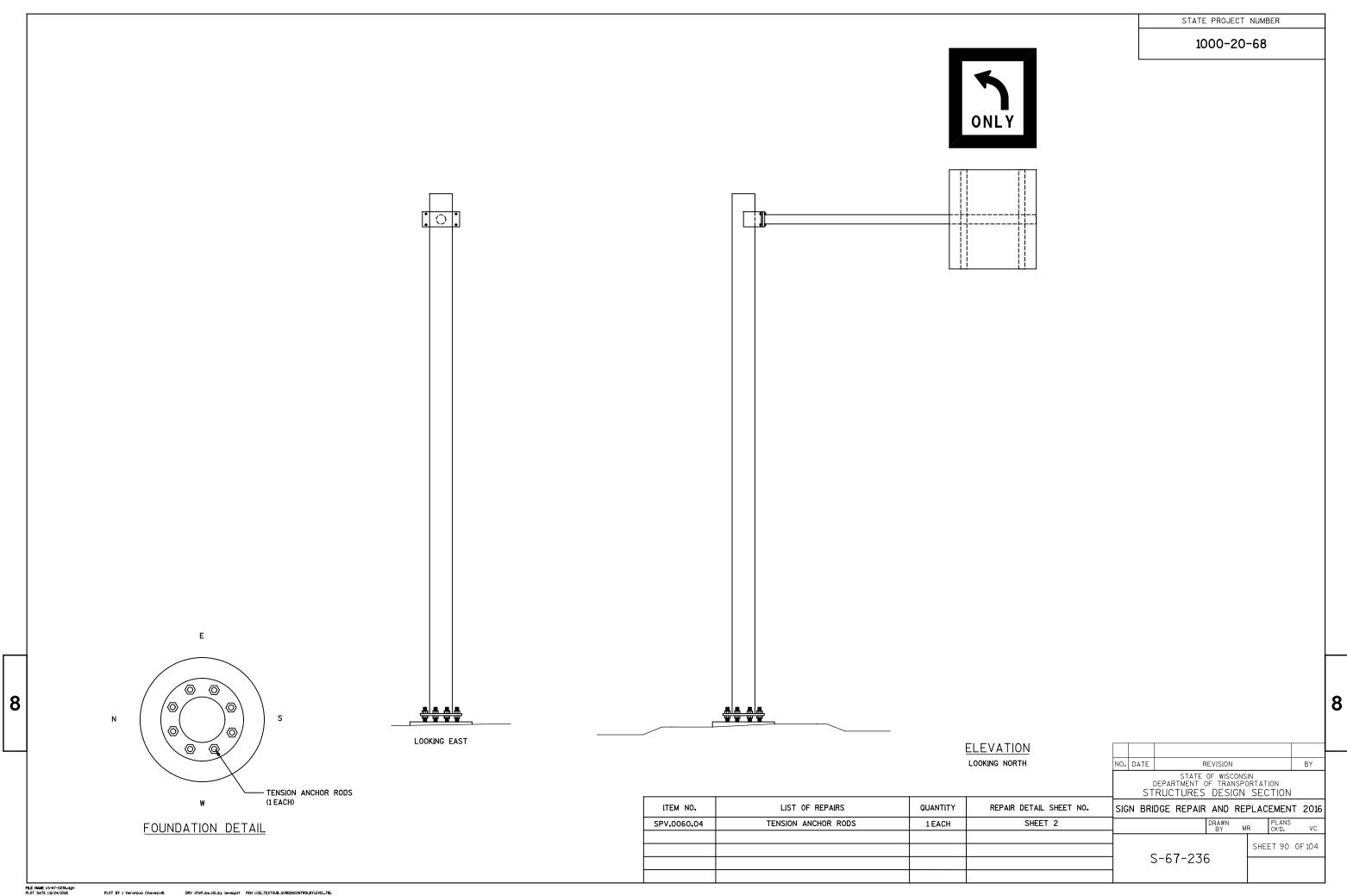
STATE PROJECT NUMBER 1000-20-68 - INSTALL SIGN CONNECTION ASSEMBLY (1 EACH) TIGHTEN/REPLACE POST TO MAST— ARM CONNECTION (1 EACH) ELEVATION LOOKING EAST LOOKING SOUTH 8 Ε NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.08 TIGHTEN/REPLACE POST TO MAST ARM CONNECTION 1 EACH SHEET 4 FOUNDATION DETAIL SPV.0060.20 REPLACE SIGN CONNECTION ASSEMBLY 1 EACH SHEET 7 SHEET 85 OF 104 S-66-201 FILE NAME : S-66-0201.dgn PLOT DATE : 11/24/2015

STATE PROJECT NUMBER 1000-20-68 -INSTALL OVERHEAD SIGN MOUNTING SYSTEM **ELEVATION** LOOKING EAST LOOKING NORTH 8 Ε -TENSION ANCHOR RODS (1 EACH) NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.04 TENSION ANCHOR RODS 1 EACH SHEET 2 SPV.0060.20 INSTALL OVERHEAD SIGN MOUNTING SYSTEM 1 EACH SHEET 6 SHEET 86 OF 104 FOUNDATION DETAIL S-67-215 FILE NAME : S-67-0215.dgn PLOT DATE : 11/24/2015 PLOT BY: Veronica Chavezv8l DRV: Pdf_bw_HS_by level.plt PEN: CEL_TEXTSUB_SCREENCONTROLBYLEVEL_TBL

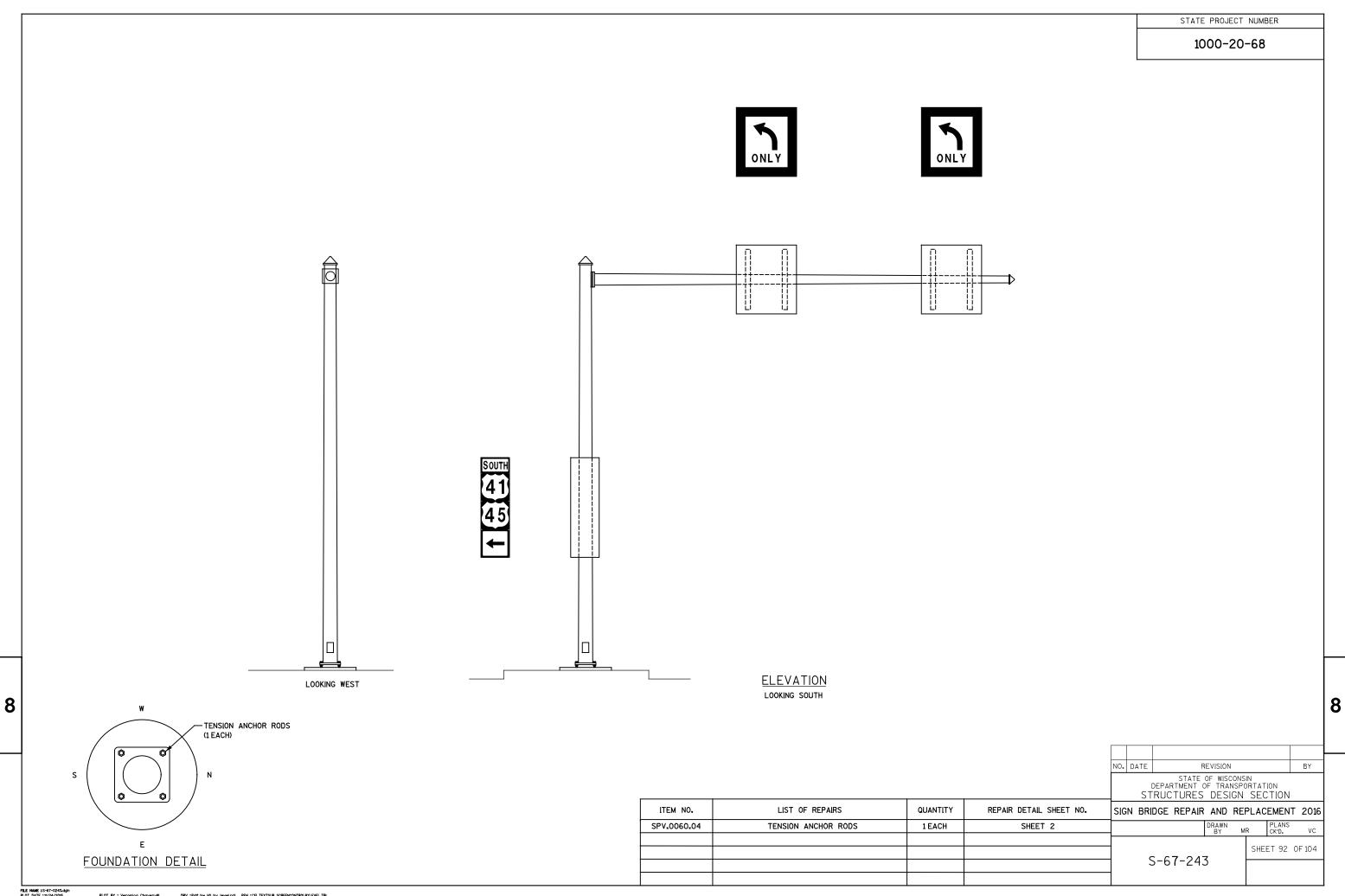


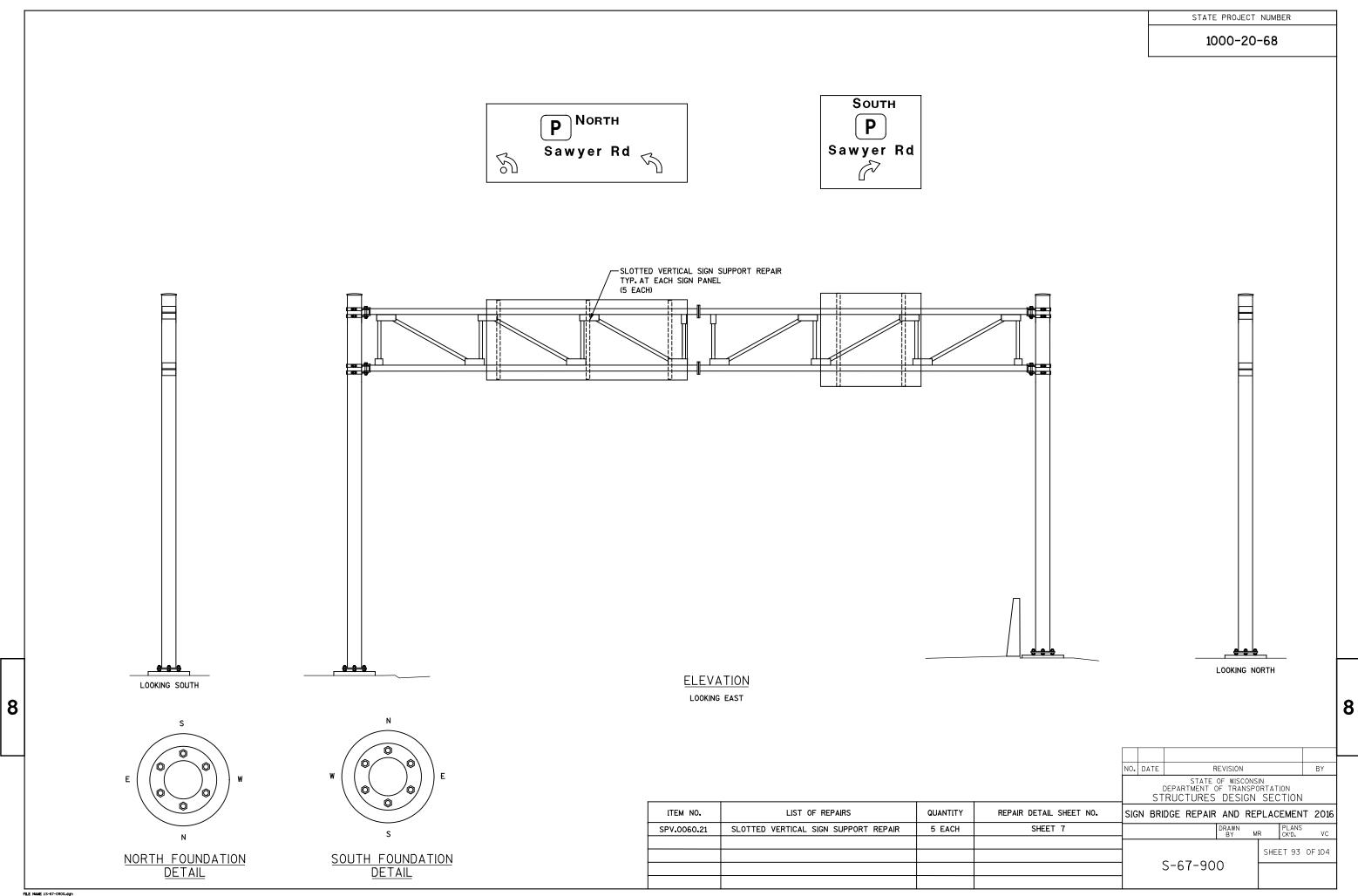


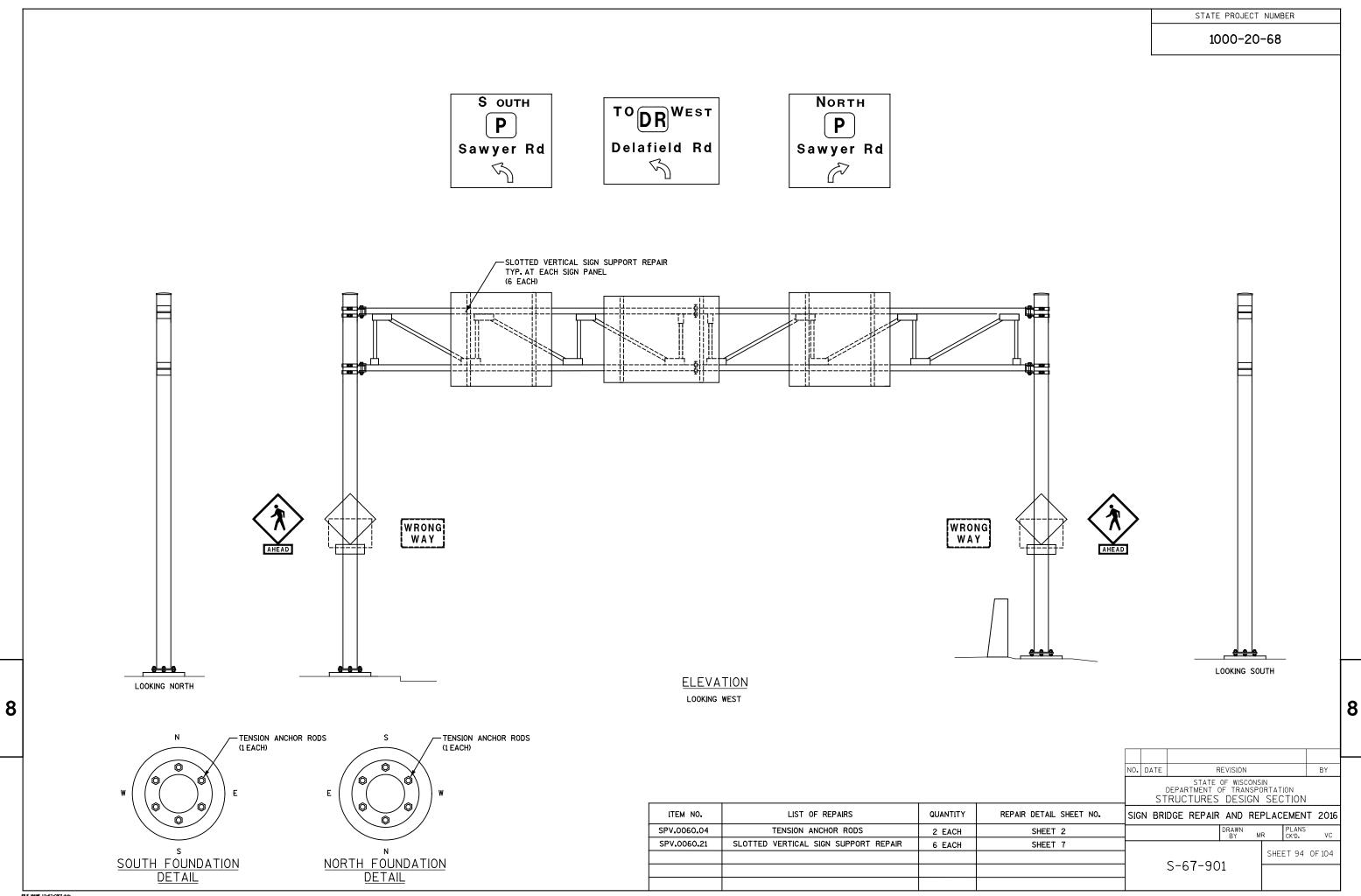


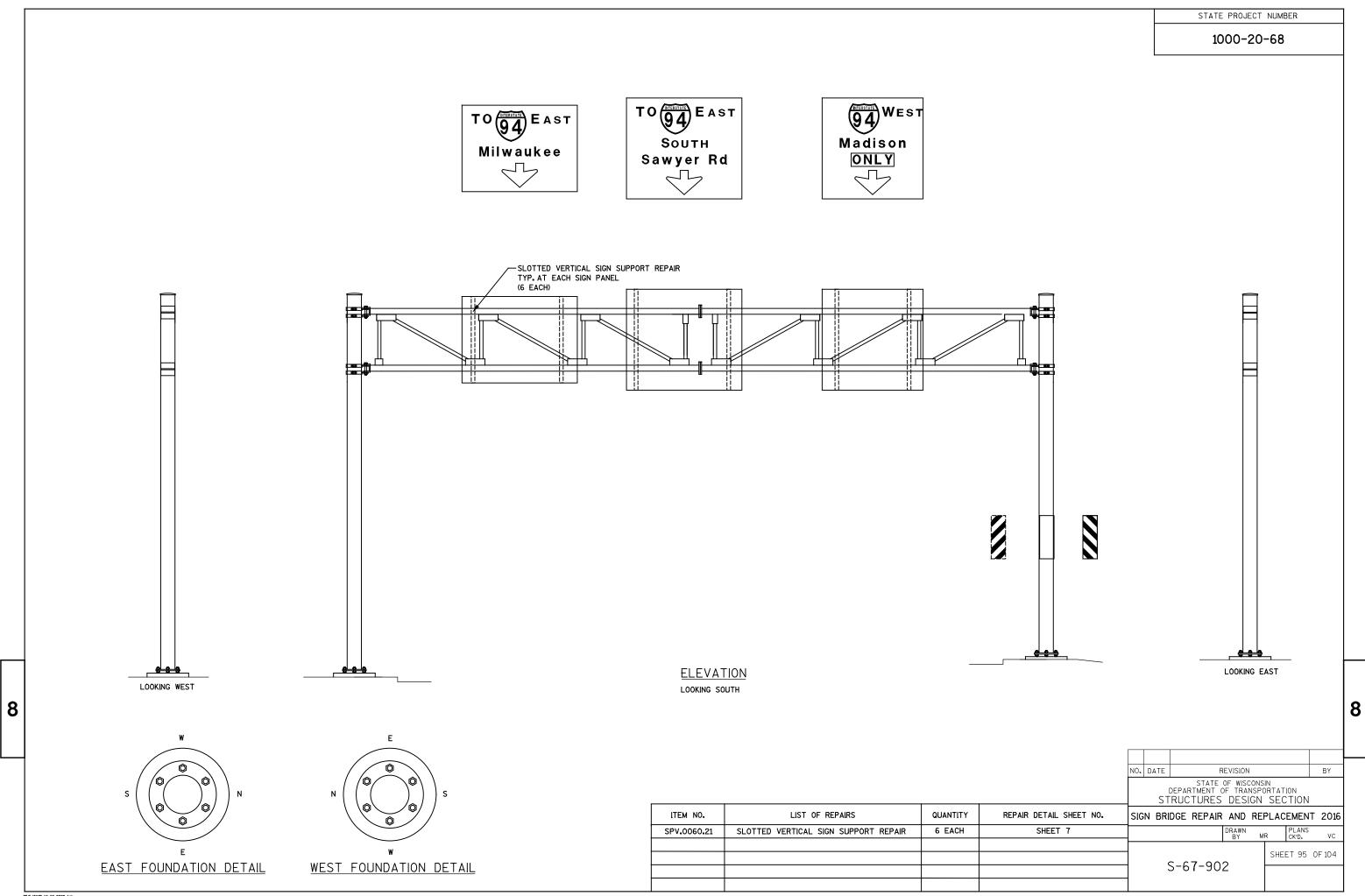


STATE PROJECT NUMBER 1000-20-68 ELEVATION LOOKING SOUTH 8 LOOKING WEST SIGN STRUCTURE ANCHOR ROD REPAIR (1 EACH) NO. DATE REVISION STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 -TENSION ANCHOR RODS SIGN STRUCTURE ANCHOR ROD REPAIR SHEET 3 SPV.0060.01 1 EACH (1 EACH) TENSION ANCHOR RODS SHEET 2 SPV.0060.04 1 EACH SHEET 91 OF 104 FOUNDATION DETAIL S-67-238 FILE NAME : S-67-0238.dgn PLOT DATE : 11/24/2015 PLOT BY: Veronica Chavezv8i DRV: Pdf_bw_HS_by level.pit PEN: CELTEXTSUB_SCREENCONTROLBYLEVEL.TBL

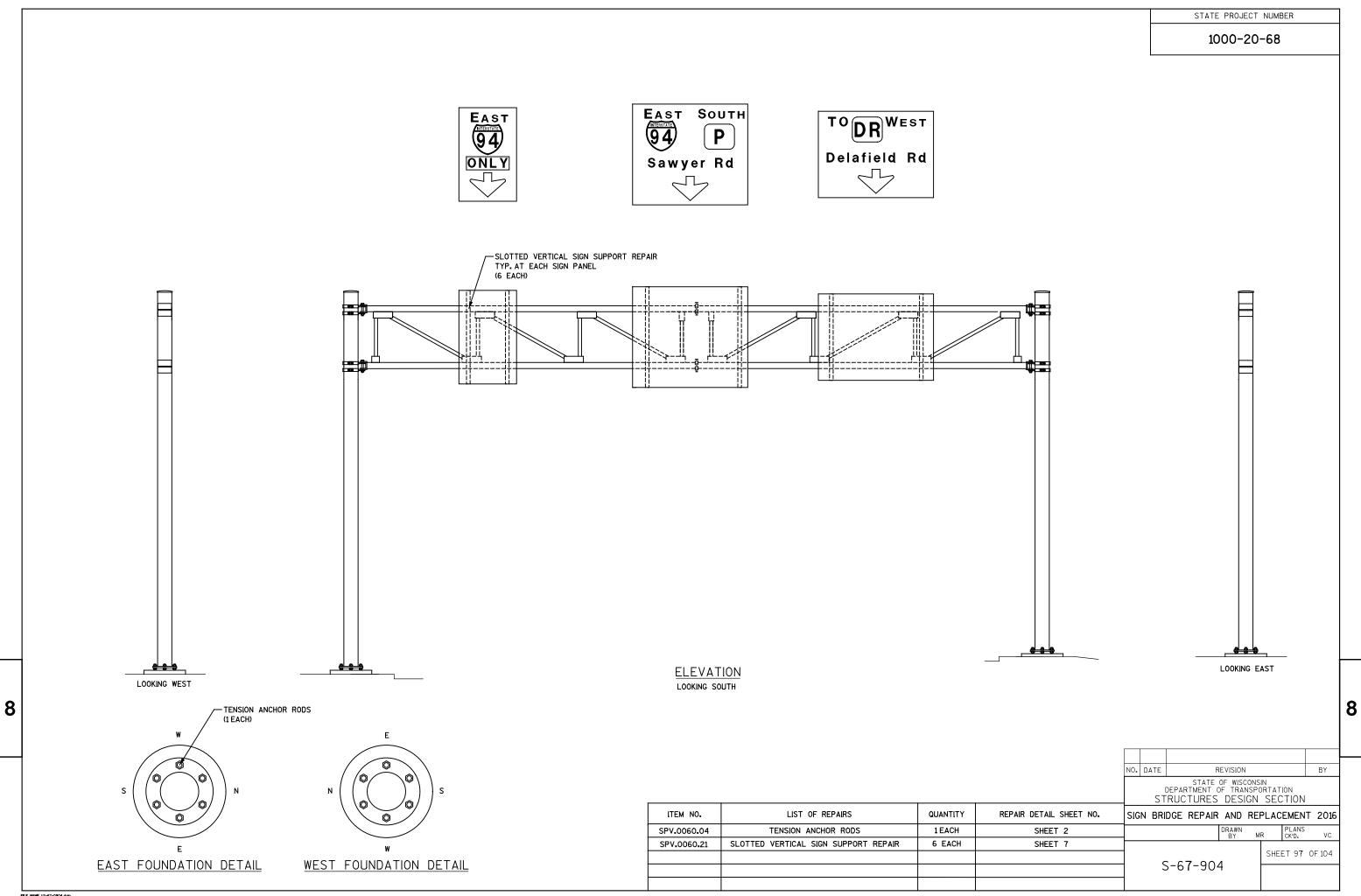


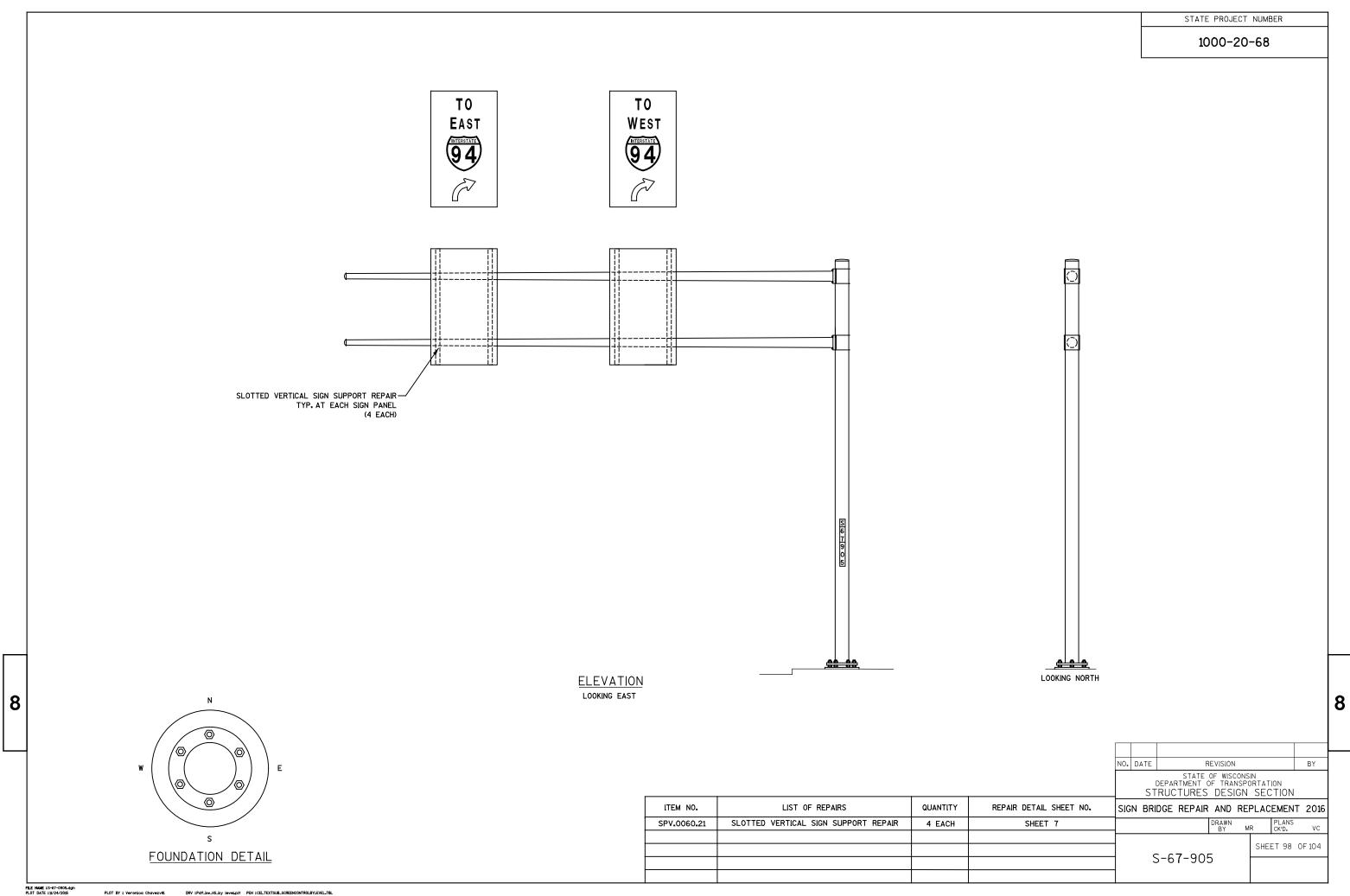


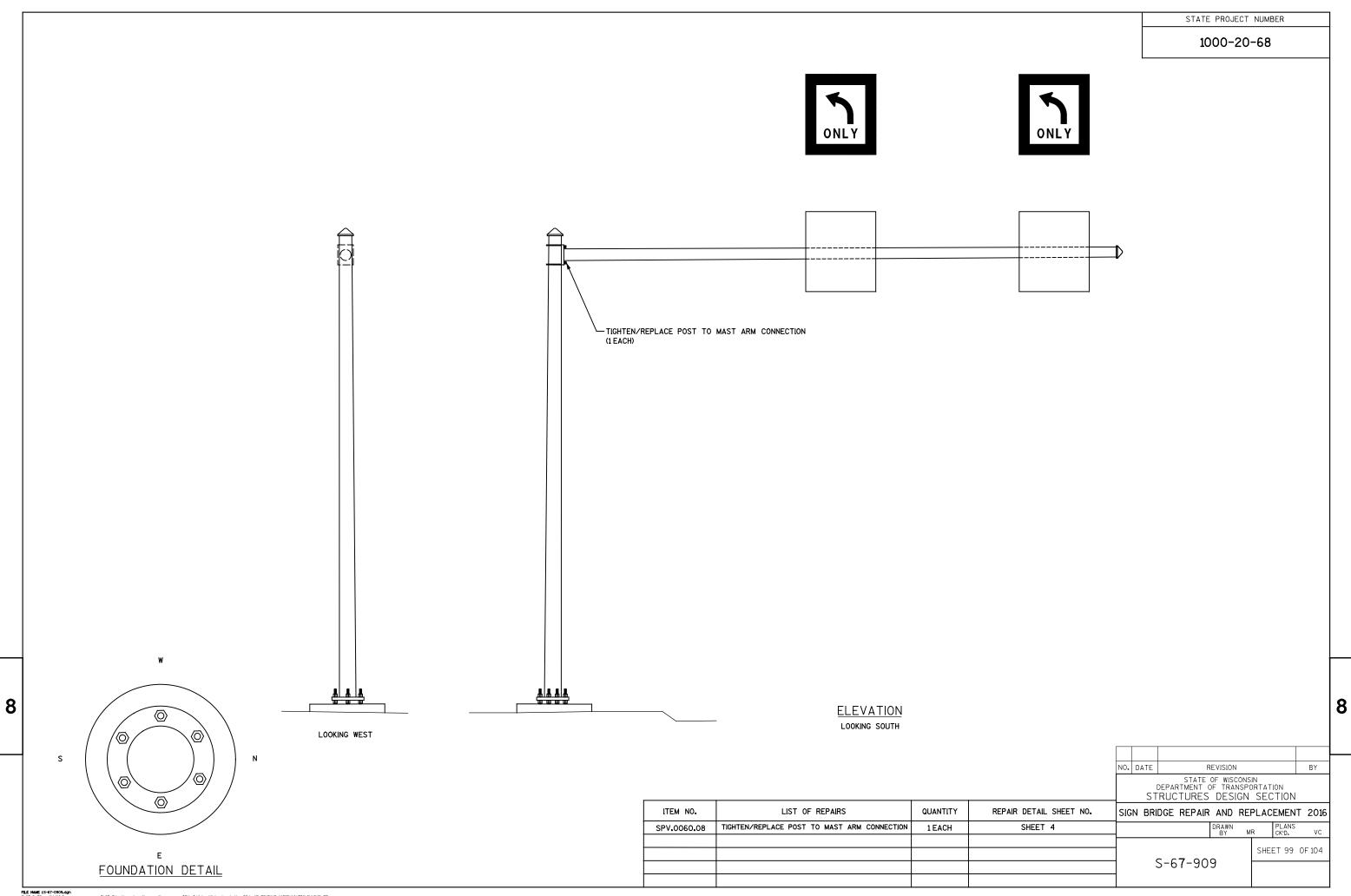


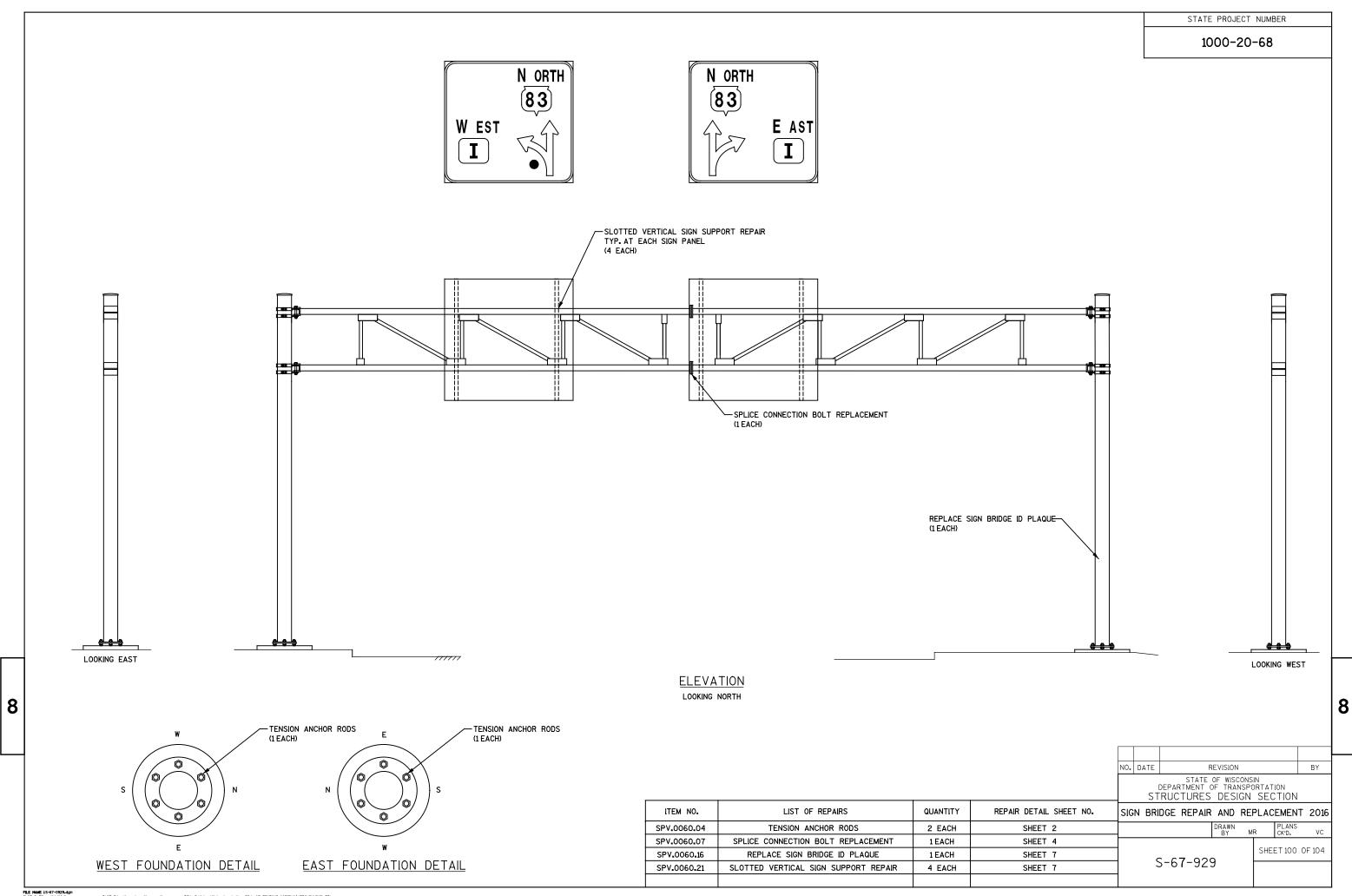


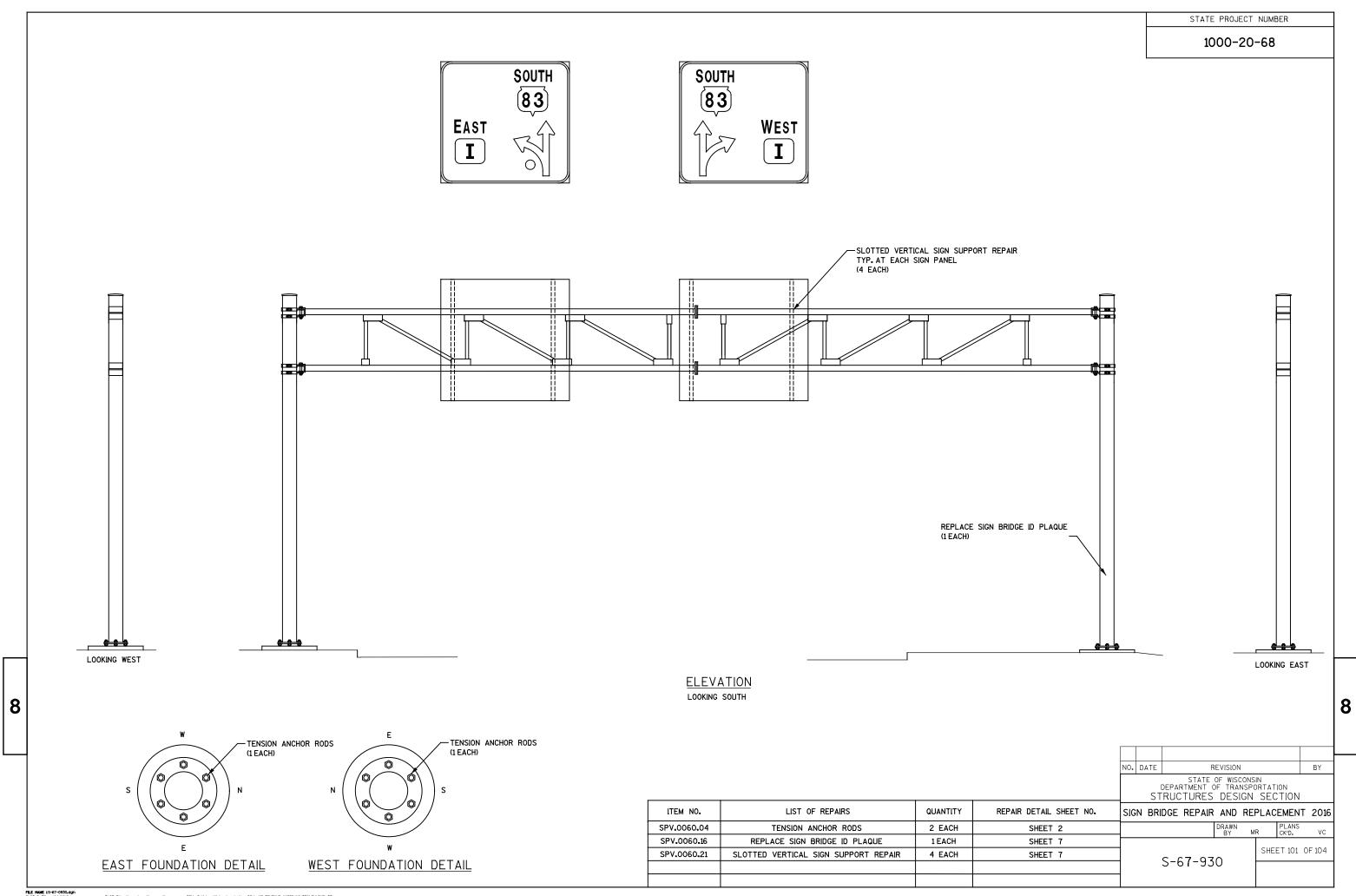
STATE PROJECT NUMBER 1000-20-68 EAST Milwaukee ONLY Madison — SLOTTED VERTICAL SIGN SUPPORT REPAIR TYP.AT EACH SIGN PANEL (6 EACH) - 0 - 0 - 1 **ELEVATION** LOOKING WEST LOOKING EAST LOOKING NORTH 8 STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION ITEM NO. LIST OF REPAIRS QUANTITY REPAIR DETAIL SHEET NO. SIGN BRIDGE REPAIR AND REPLACEMENT 2016 SPV.0060.21 SLOTTED VERTICAL SIGN SUPPORT REPAIR 6 EACH SHEET 7 SHEET 96 OF 104 S-67-903 WEST FOUNDATION DETAIL EAST FOUNDATION DETAIL

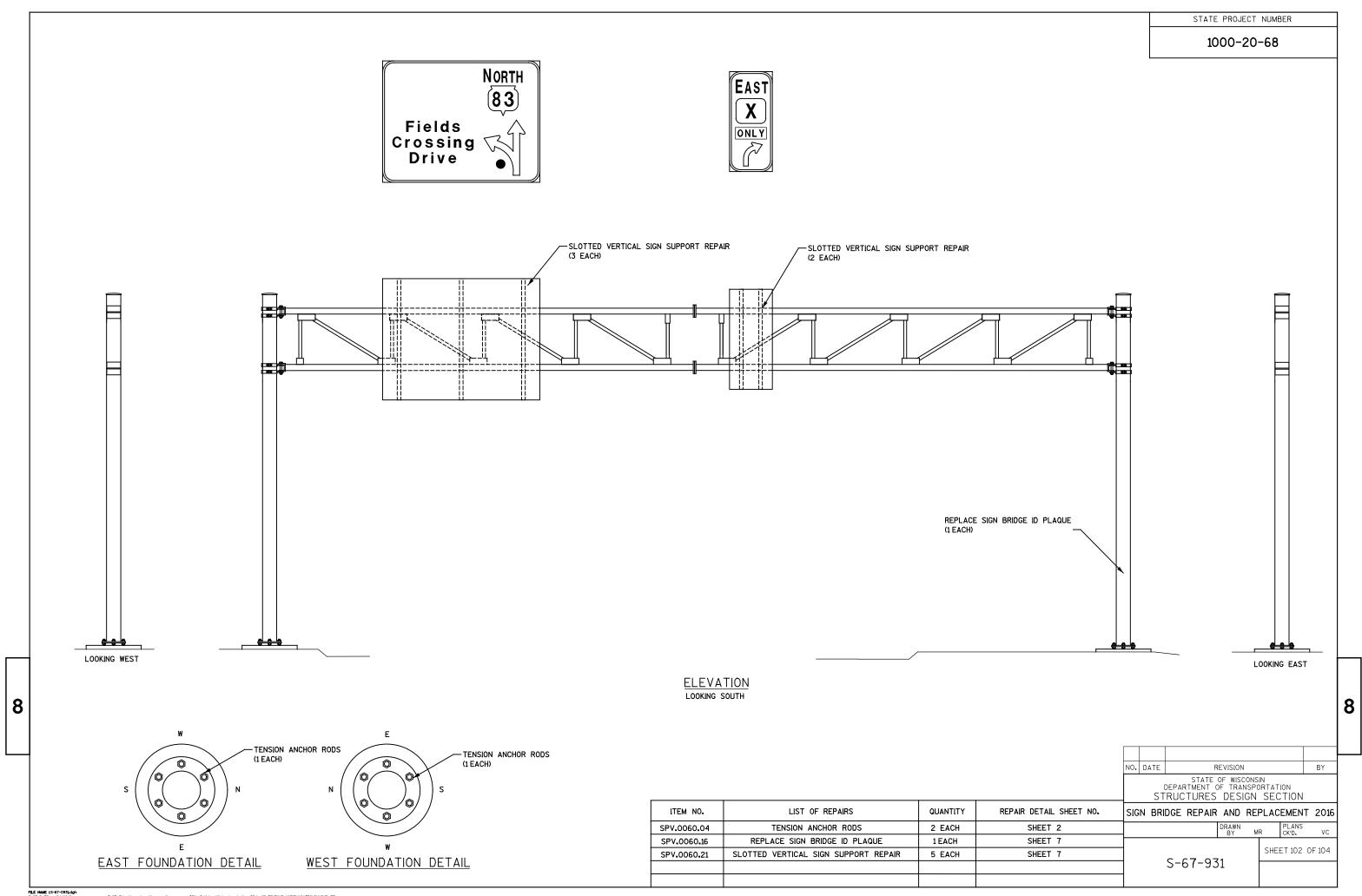








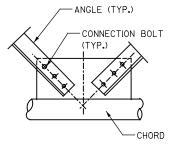




STATE PROJECT NUMBER

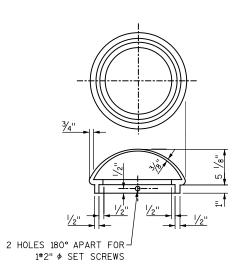
1000-20-68

	REPLACE DISHED WASHER	REPLACE MISSING HANDHOLE COVER	REPLACE SAFETY CHAIN	REPLACE TRUSS MEMBER	SECURE/REPLAŒ VERTICAL POST CAP	TENSION CANTILEVER POST TO TRUSS CONNECTION BOLT	TIGHTEN LOOSE U-BOLT	TIGHTEN/ REPLACE HANDHOLE COVER BOLT	ALIGN LOCKING PIN	TENSION TRUSS GUSSET CONNECTION BOLT	INSTALL/REPLACE CATWALK HANDRAIL
	SPV.0060.31	SPV.0060.32	SPV.0060.33	SPV.0060.34	SPV.0060.35	SPV.0060.36	SPV.0060.37	SPV.0060.38	SPV.0060.39	SPV.0060.40	SPV.0090.01
	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	LF
UNDISTRIBUTED	4	2	2	1	2	2	4	2	2	2	4
TOTAL	4	2	2	1	2	2	4	2	2	2	4

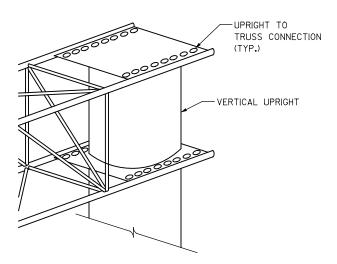


TRUSS GUSSET CONNECTION DETAIL

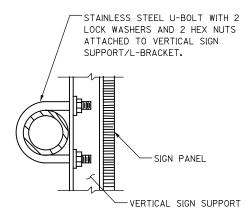
__ANGLE (TYP.)



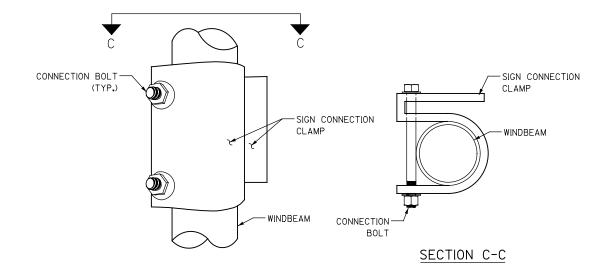
POST CAP DETAIL



CANTIEVER SIGN BRIDGE
POST TO TRUSS CONNECTION DETAIL

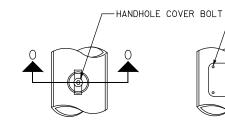


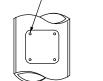
INSTALL U-BOLT & TIGHTEN
LOOSE U-BOLT DETAIL



HANDHOLE COVER 3/8"

SECTION 0-0





-HANDHOLE COVER BOLT

HANDHOLE DETAILS

NO.	DATE	F	EVISION			BY
			OF WISC		IONI	
DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN SECTION						
SIGN BRIDGE REPAIR AND I		REPLACE	EMENT	2016		
			DRAWN	MR	PLANS	RW

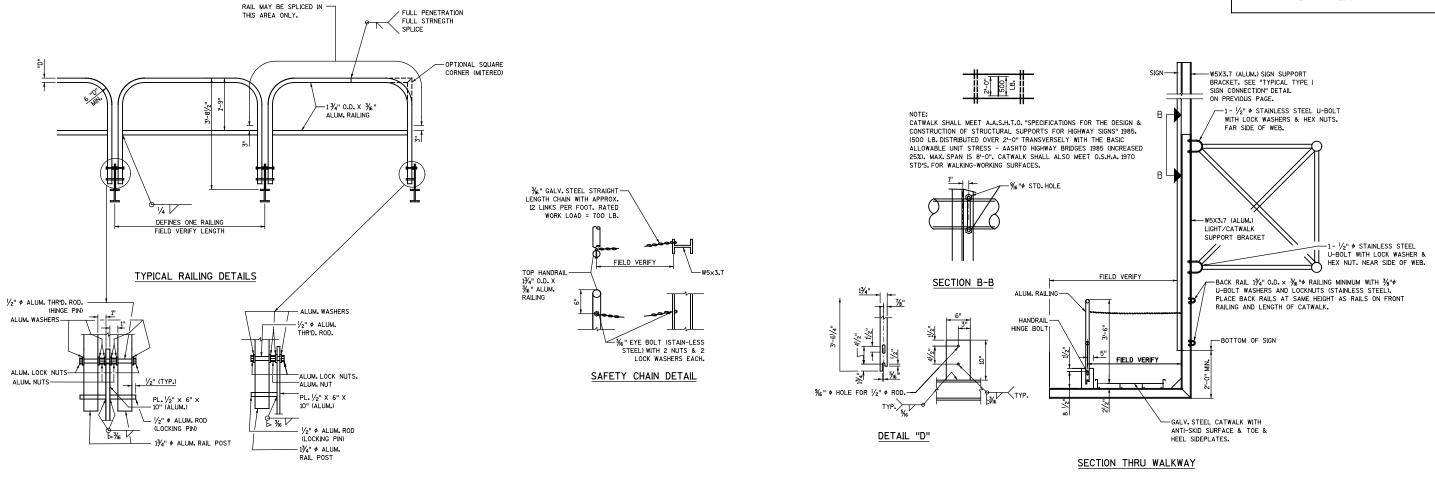
8

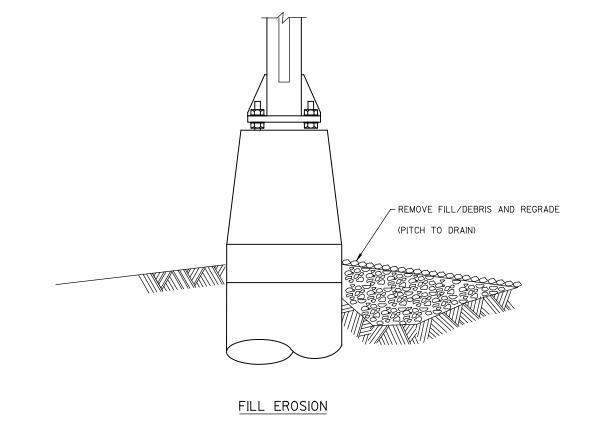
UNDISTRIBUTED SHEET 103 OF 104 QUANTITIES DETAILS (1 OF 2)

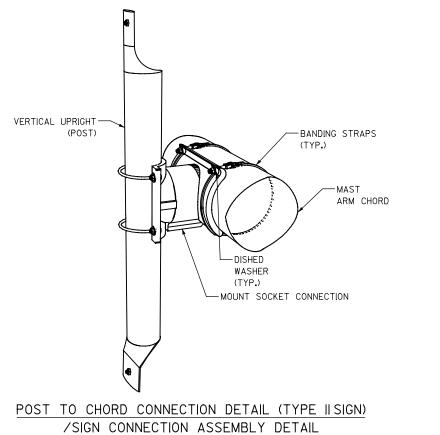
SIGN CONNECTION CLAMP DETAIL (TYPE 2)



1000-20-68







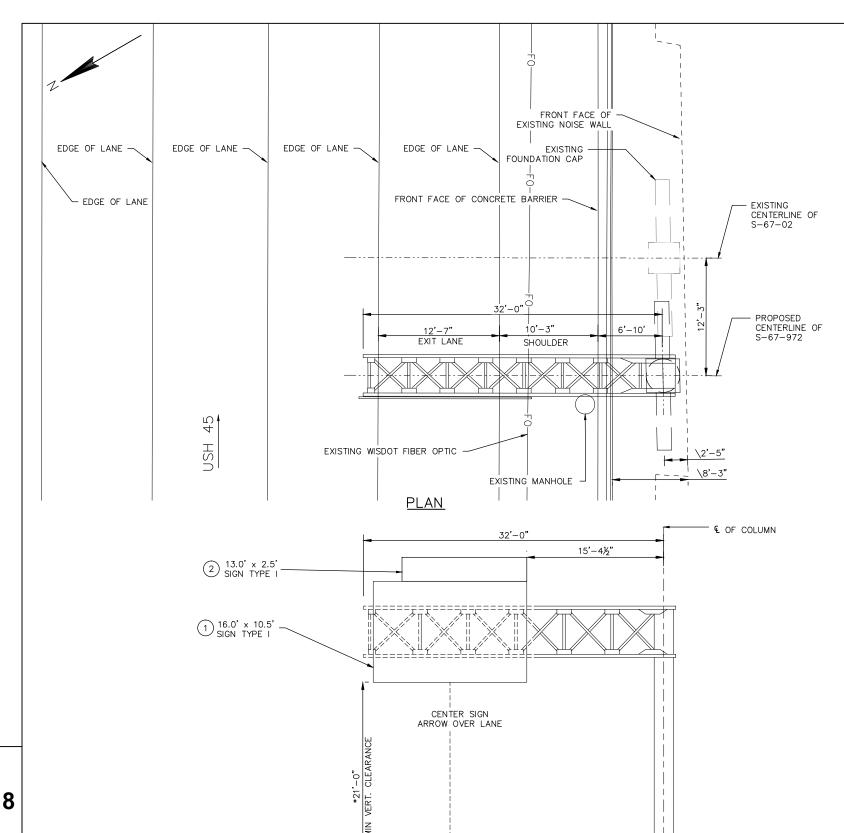
NO. DATE STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
STRUCTURES DESIGN SECTION

8

SIGN BRIDGE REPAIR AND REPLACEMENT 2016

UNDISTRIBUTED SHEET 104 OF 104 QUANTITIES DETAILS (2 OF 2)

8



EXIT LANE

TO STH 100

HIGH POINT ELEV.

LIST OF DRAWINGS

- 1. PLAN & ELEVATION
- 2. CANTILEVER TRUSS FOOTING
- REMOVAL DETAILS
- 4. GALVANIZED STEEL CANTILEVER SIGN TRUSS
- 5. GALVANIZED STEEL CANTILEVER SIGN TRUSS DETAILS
- 6. HANDHOLE DETAILS

MATERIAL PROPERTIES:

CONCRETE: $f c = 3,500 \text{ psi}$
HIGH STRENGTH BAR
STEEL REINFORCEMENT:fy = 60,000 psi
OTES, COLUMN A CHORDO
STEEL COLUMN & CHORDS:fy = 42,000 psi
(INCLD. HANDHOLE) A.P.I. SPEC 5L GRADE X42
PLATES, BARS, STRUCTURAL ANGLES:fy = 36,000 psi
A.S.T.M. A709 GRADE 36
STEEL ANCHOR BOLTS:fy = 55,000 psi
ASTM F1554 GRADE 55.
ASTM A563A HEAVY HEX NUTS.
The time the time to the time
AND ASTM F436 WASHERS.
HIGH STRENGTH BOLTS A325fy = 92,000 psi
STRUCTURAL MEMBERS GALVANIZED A123
HARDWARE GALVANIZEDA153 CLASS C

DEAD LOAD - 3 PSF OF SIGN. WEIGHT OF SUPPORTING STRUCTURE, CATWALK, LIGHTS, AND RAILINGS.

LIVE LOAD - NONE.

ICE LOAD - 3 PSF TO 1 FACE OF SIGN & AROUND SURFACE OF MEMBERS.

WIND PRESSURE - 90 M.P.H. (3-SECOND GUST SPEED) TO SIGN AREA & EXPOSED MEMBERS.

FATIGUE CATEGORY I WITHOUT GALLOPING WIND EFFECTS.

ALL BOLTED CONNECTIONS SHALL BE MADE WITH 3" DIAMETER A325 BOLTS, GALVANIZED ASTM A153, CLASS C, AND INSTALLED WITH DTI WASHERS.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

ALL ELEVATIONS ARE BASED ON NGVD 29 UNLESS OTHERWISE SHOWN OR NOTED. $\,$

THE SIGN BRIDGE WAS DESIGNED ACCORDING TO THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINARIES, AND TRAFFIC SIGNALS.

STATE PROJECT NUMBER

1000-20-68

ALL STRUCTURAL STEEL MEMBERS SHALL BE GALVANIZED.

CENTER SIGNS VERTICALLY ON CHORD/TRUSS.

CENTER SIGNS OVER RESPECTIVE LANE IF THEY HAVE A DOWN ARROW.

ALTERNATE DESIGNS ARE NOT ALLOWED.

THE LOCATION OF EXISTING OR PROPOSED UTILITIES AS NOTED ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. VERIFY UTILITY INTERFERENCES PRIOR TO CONSTRUCTION.

PROVIDE VENTILATING RODENT SCREENS AT ALL COLUMN BASES (NO GROUT) CONSIDERED INCIDENTAL TO BID ITEM "SIGN BRIDGE CANTILEVERED S-67-972".

PROVIDE A 34" CHAMFER OR 1" RADIUS ON ALL EXPOSED CONCRETE EDGES.

PROVIDE HANDHOLES ON UPRIGHT.

THE UPPER 12" OF ANCHOR BOLTS, NUTS AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE AASHTO SPECIFICATION AS STATED IN SECTION 641. OF THE WISDOT STANDARD SPECIFICATIONS

PROVIDE AN IDENTIFICATION PLAQUE FOR THE SIGN BRIDGE IN ACCORDANCE WITH SDD STRUCTURE IDENTIFICATION PLAQUES, SIGN BRIDGES AND OVERHEAD SIGN SUPPORT. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "SIGN BRIDGE CANTILEVERED S-67-972"

SIGNS OR BLANKS SHALL BE INSTALLED ON THE TRUSS AT THE TIME OF ERECTION. SIGNS SHALL BE AS DESIGNATED IN PLANS. SIGN BLANKS AND MOUNTING HARDWARE SHALL BE INCIDENTAL TO SIGN BRIDGE CANTILEVERED S-67-972

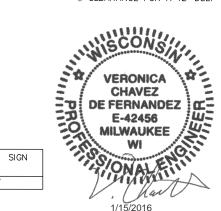
INSTALL CONDUIT PLUGS IN ALL UNUSED CONDUIT HOLES, CONSIDERED INCIDENTAL TO THE BID ITEM "SIGN BRIDGE CANTILEVERED S-67-972"

WELD TEST AS PER AWS D1.1

REPRESENTS THE SIGN NO. REFER TO THE PERMANENT SIGNING SHEETS

SUBMIT SHOP DRAWINGS TO TOM HEYDEL, SE REGION WISDOT TRAFFIC OPS.

* CLEARANCE FOR A 12' DEEP SIGN IS PROVIDED FOR FUTURE PROVISIONS.



TYPE I SIGN DESIGN DATA

MAX DESIGN SIGN AREA	MAX. TYPE I SIGN				
(SQ. FT.)	DEPTH				
264	12'-0"				

ESTIMATE OF QUANTITIES

ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL
636.0100	SIGN SUPPORTS CONCRETE MASONRY	CY	8
636.1000	SIGN SUPPORTS STEEL REINFORNCEMENT HS	LB	980
641.1200	SIGN BRIDGE CANTILEVERED S-67-972	LS	1
SPV.0105.03	PARTIAL REMOVAL OF SIGN STRUCTURE FOUNDATION	LS	1

NO. DATE REVISION

COLLINS ENGINEERS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION 8 ACCEPTED William C. Drehe SDR 01/21/16 CHIEF STRUCTURES DESIGN ENGINEER

> USH 45 SB AT EXIT TO STH 100 S-67-972

TOWN/CITY/VILLAGE MENOMONEE FALL AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS DESIGNED PLANS MR CK'D. DESIGN VC CK D.

> PLAN & **ELEVATION**

SHEET 1 OF 6

SE REGION CONTACT: TOM HEYDEL (262) 548-6763

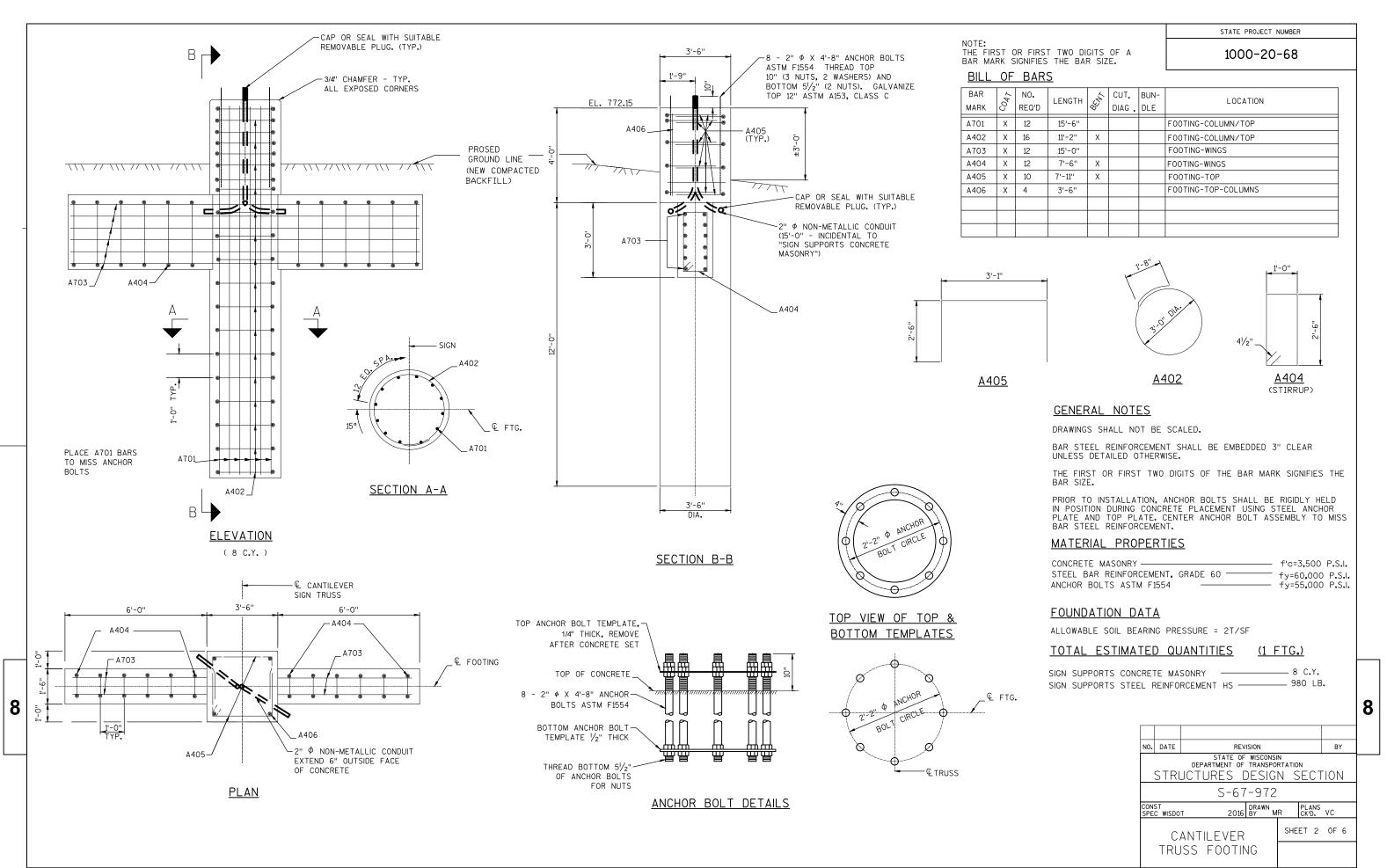
ELEVATION

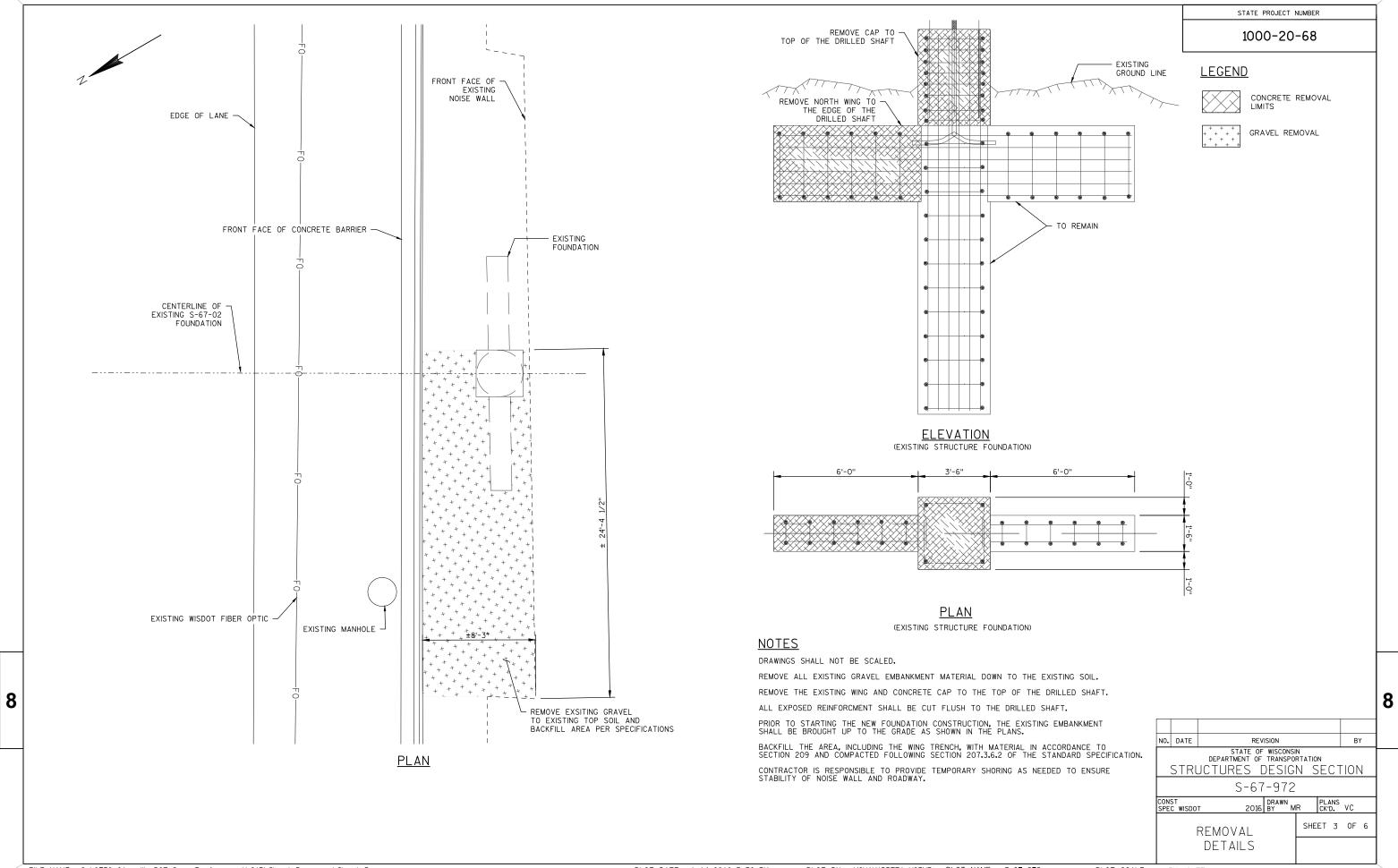
(LOOKING SOUTH AT FRONT FACE OF SIGN) US 45 SB AT EXIT TO STH 100 S-67-972 TO REPLACE S-67-02

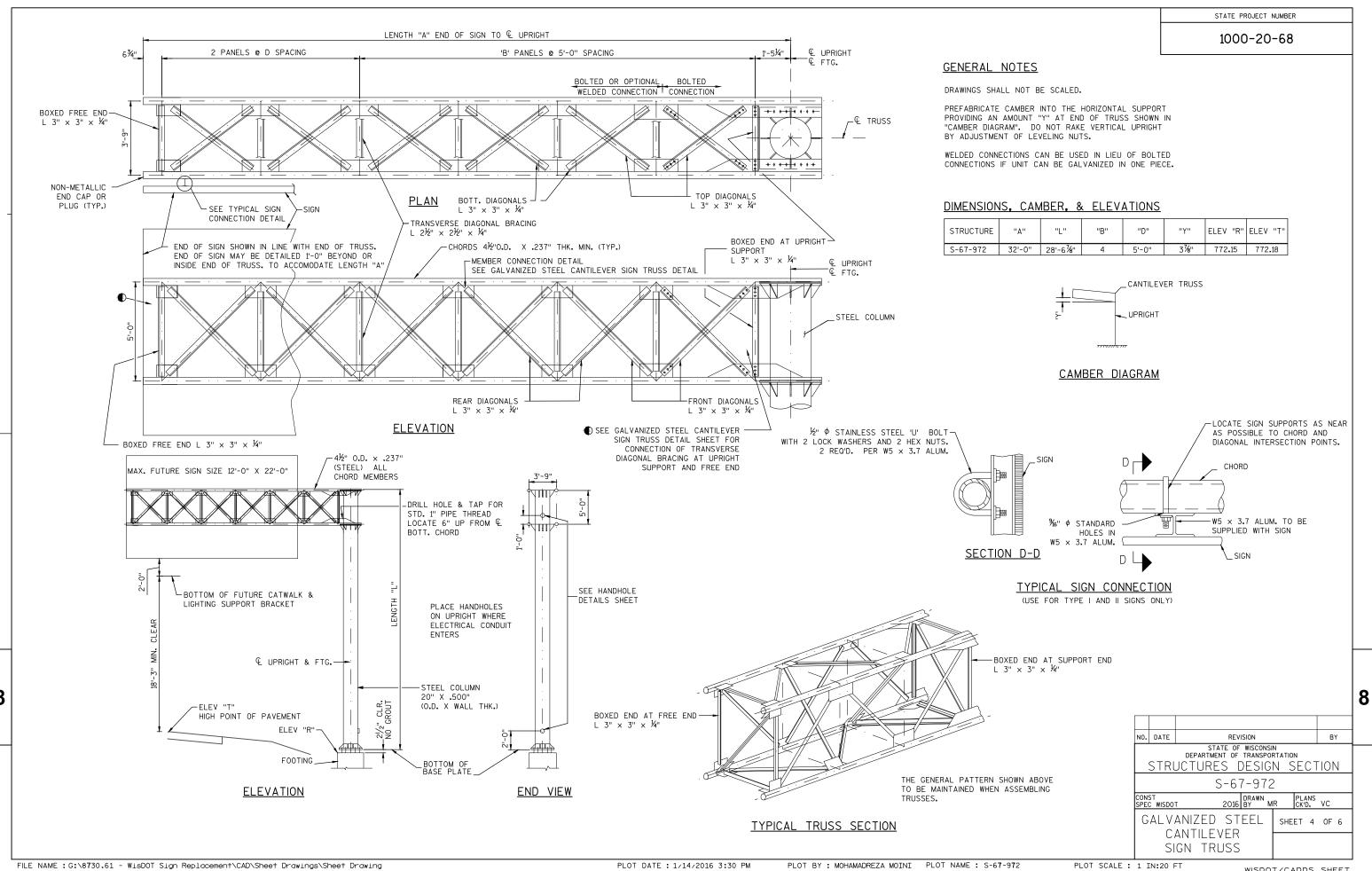
46'-0" USH 45 SB

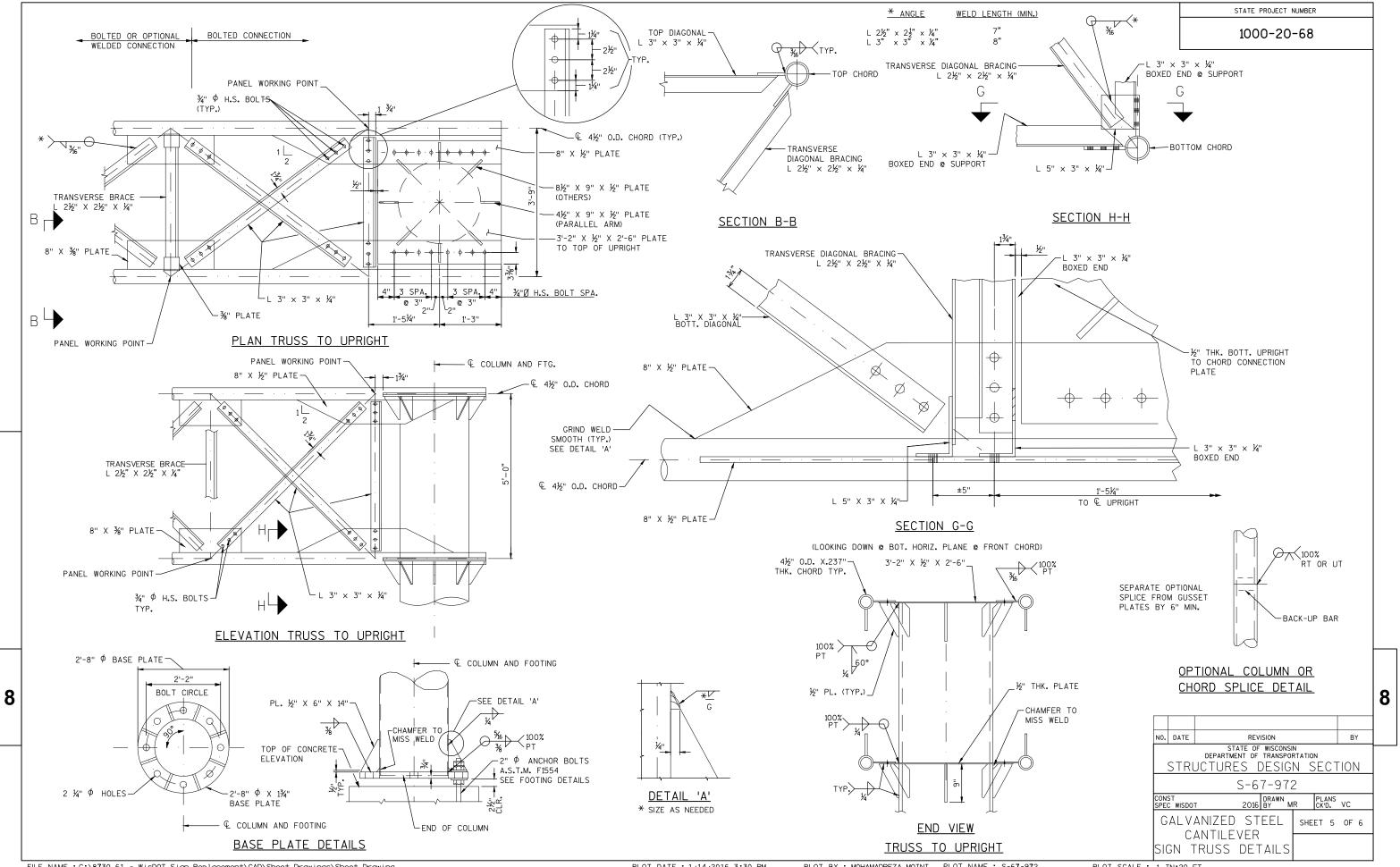
6'-10"

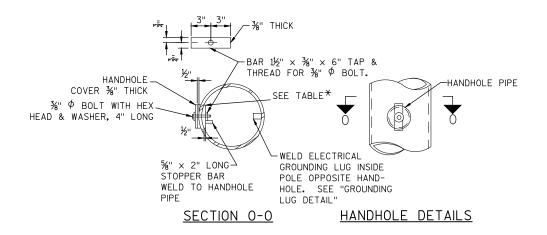
SHOULDER







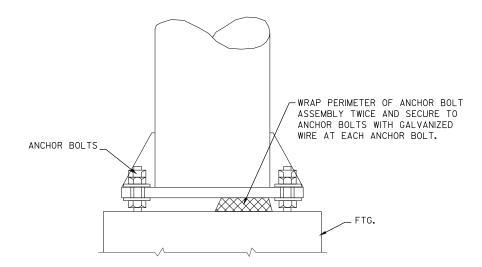




HANDHOLE NOTES

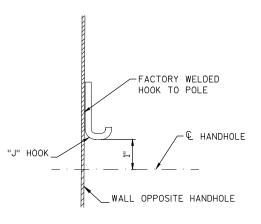
HANDHOLES SHALL BE LOCATED IN ONE COLUMN OF THE SIGN BRIDGE STRUCTURE IF ELECTRICALLY OPERATED DEVICES ARE INSTALLED ON/IN THE STRUCTURE. COLUMNS WITH HANDHOLES SHALL BE NEAR THE ELECTRICAL SERVICE. THE CONTRACTOR SHALL VERIFY THE LOCATION OF THE ELECTRICAL SERVICE ENTRANCE WITH THE REGION TRAFFIC SECTION PRIOR TO FABRICATION OF THE SIGN BRIDGE COLUMNS AND MEMBERS. CONDUIT (AS REQ'D.) SHALL BE LOCATED, PLACED AND SIZED AS SHOWN ON THE ELECTRICAL DETAIL PLAN SHEETS.

*	COLUMN SIZE O.D. X THICKNESS	HANDHOLE PIPE O.D. X MIN. THK.				
	UP TO AND INCLD. 16" X .375"	5.562" X .500"				
	GREATER THAN 16" X .375" TO AND INCLD. 24" X .562"	6.625" X .562"				



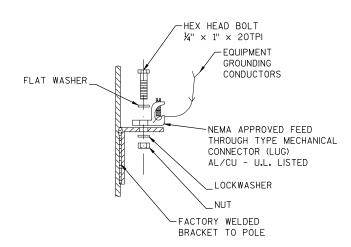
RODENT SCREEN

(ONLY REQ'D. WHEN ELECTRICAL DEVICES ARE INSTALLED)



TYPICAL "J" HOOK LOCATION

THE "J" HOOK SHALL BE FACTORY WELDED TO THE INSIDE OF ALL COLUMNS CONTAINING ELECTRICAL WIRING. THE "J" HOOK SHALL BE ATTACHED ABOVE THE CENTERLINE OF THE UPPER HANDHOLE AND MOUNTED DIRECTLY OPPOSITE THE HANDHOLE AS SHOWN IN THE DRAWING.



GROUNDING LUG DETAIL

NUT, BOLT, AND WASHERS SHALL BE STAINLESS STEEL.



8

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

http://www.dot.wisconsin.gov