EAU NOVEMBER 2015

ORDER OF SHEETS

Section No	. 1	Title
Section No	. 2	Typical Sections and Details
Section No	. 3	Estimate of Quantitles
Section No	. 3	Miscellaneous Quantitles
Section N o	. 4	Right of Way Plat
ection No	. 5	Plan and Profile
ection No	. 6	Standard Detall Drawings
ection No	. 7	Sign Plates
lection N o	. 8	Structure Plans

TOTAL SHEETS = 188

Section No. 9 Cross Sections

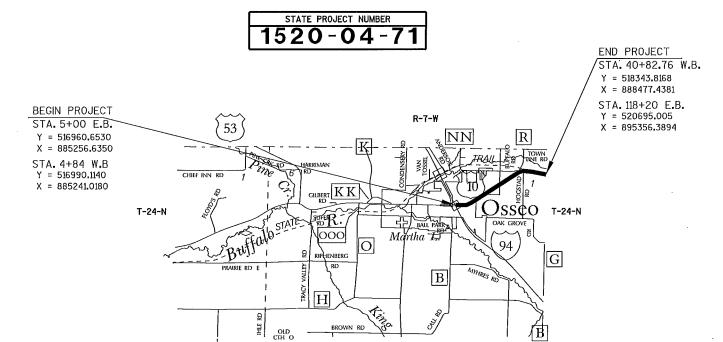
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

OSSEO - FAIRCHILD

IH 94 TO EAST COUNTY LINE

USH 10 TREMPEALEAU



R-7-W

CONTRACT

FEDERAL PROJECT

PROJECT

WISC 2015584

STATE PROJECT

1520-01-71

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PREPARED BY Designer Project Manager Regional Examiner ___ Regional Supervisor C.O. Examiner

CONVENTIONAL SYMBOLS

DESIGN DESIGNATION

= 5262

= 55 M.P.H.

A.A.D.T.

A.A.D.T. D.H.V. D.D.

ESALS

MARSH AREA

WOODED OR SHRUB AREA

DESIGN SPEED

CONTENTIONAL STRIDGES	
PLAN	,,,
CORPORATE LIMITS PROPERTY LINE	<u>///</u>
LOT LINE	
LIMITED HIGHWAY EASEMENT	L_
EXISTING RIGHT OF WAY PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	-căi
	7

PROFILE GRADE LINE ORIGINAL GROUND MARSH OR ROCK PROFILE (To be noted as such) SPECIAL DITCH GRADE ELEVATION CULVERT (Profile View) UTILITIES ELECTRIC FIBER OPTIC

STORM SEWER

TELEPHONE WATER

POWER POLE

SANITARY SEWER UTILITY PEDESTAL TELEPHONE POLE

LAYOUT 6.25 MI. TOTAL NET LENGTH OF CENTERLINE = 2.15 MJ.

HORIZONTAL POSITIONS SHOWN ARE WISCONSIN COUNTY COORDINATES, TREMPEALEAU COUNTY, NAD83 (2010, IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

FILE NAME: C:\Users\dotJdb\Desktop\1520_04_01_ti.dgn

PLOT DATE: 21-APR-2015 15:40

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

PLOT SCALE: 200.000000:1.000000

LIST OF STANDARD ABBREVIATIONS

ABUTMENT ABUT. AGG. AGGREGATE AHEAD **APPROXIMATE** APPROX APRON ENDWALL A.F.W ASPHALTIC ASPH AVERAGE DAILY TRAFFIC A.D.T AZIMUTH BACK BEGIN BEG BENCH MARK R M CENTER LINE CONST. CONSTRUCTION COUNTY COUNTY TRUNK HIGHWAY CROSS SECTION X-SEC. CR. CFS C.Y., CU. YD. CUBIC FEET/SECOND CUBIC YARD CULVERT CULV. C.P. CULVERT PIPE DEPARTMENT OF TRANSPORTATION D.O.T. D.H.V. DESIGN HOUR VOLUME DIA. DIAMETER DIRECTIONAL DISTRIBUTION DISCH. OR DIS DISCHARGE ELECTRIC ELECT EL. (EMB. OR ELEV. FIFVATION **EMBANKMENT** EXCAVATION BELOW SUBGRADE E.B.S **FXIST** FERTILIZE FERT. FIELD ENTRANCE FIN. FT. FINISHED FOOT FLOW LINE F.L. GA. GAUGE HORIZ. HORIZONTAL CWT. HUNDREDWEIGHT INL. INLET LT. LEFT L.H.F. LEFT-HAND FORWARD LINEAR LIN. FT LINEAR FOOT LUMP SUM MAX. MAXIMUM ML MISC. MISCELLANEOUS N.E. NORTH WEST PAV'T PAVEMENT PC POINT OF CURVATURE POINT OF INTERSECTION P.I. POINT OF TANGENCY P.O.T. POINT ON TANGENT POUND PRIVATE ENTRANCE PROJ. PROJECT RANGE REQ'D REQUIRED RIGHT-HAND FORWARD R/W RIGHT OF WAY ROAD SHR. SHRINKAGE SLOPE STD. STANDARD STANDARD DETAIL DRAWINGS S.T.H. STA. S.P.P.A. STATE TRUNK HIGHWAY STATION STRUCTURAL PLATE PIPE ARCH STRUCT STRUCTURE SURFACE SURF TEL. TELEPHONE TN. TOWN TRUCKS (PERCENT OF) UNCL UNCLASSIFIED

UTILTIES

PO BOX 244

608-628-0575

imc140@frontier.com

CENTURY LINK
24 HOUR EMERGENCY REPAIR 800-824-2877
DONNA SMOTHERS
835 RED IRON ROAD
BLACK RIVER FALLS, WI 54615
715-284-4375 (OFFICE)
715-797-4854 (MOBILE)
donna.smothers@CenturyLink.com

AT@T LEGACY
ATTN: BRAD KEMPF
715-254-5238
BILL KOENIG

WE ENERGIES-GAS/PETROLEUM 24-HOUR EMERGENCY 800-261-5325 ATTN: TRAVIS KAHL 1921 8TH ST. SOUTH WISCONSIN RAPIDS, WI 54495 715-421-7256 (OFFICE) 715-498-6180 (MOBILE) travis.kahl@we-energies.com

CITY OF OSSEO - WATER

osseopublicworks@triwest.net

13712 8TH ST

P.O BOX 308

OSSEO, WI 54758

ATTN: JIM DEICH

(715) 597-2207

TRI-COUNTY COMMUNICATIONS
ATTN: BRIAN MELSNESS
23451WHITEHALL ROAD
PO BOX 727
INDEPENDENCE, WI 54747-0727
715-695-2691 (OFFICE)
715-530-0081 (MOBILE)
bmelsness@tcc.coop
WINDSTREAM NTI

LAKE MILLS, WI 53551-0244

WINDSTREAM NTI
ATTN: DENNIS RUESS
8531 CTH FF
WISCONSIN RAPIDS, WI 54494
812-456-1249 (OFFICE)
608-512-5587 (MOBILE)
dennis.ruess@kdlinc.com

EAU CLAIRE ENERGY COOPERATIVE-DISTRIBUTION ATTN: DONALD DRAEGER 8214 USH 12 PO POX 368 FALL CREEK, WI 54742-0368 715-836-6479 (OFFICE) ddraeger@ecec.com

XCEL ENERGY -DISTRIBUTION
ATTN: JOHN KELSER
1414 W HAMILTON AVE
PO BOX 8 EAU CLAIRE, WI 54702-0008
715-737-1431 (OFFICE)
john.kelser@xcelenergy.com
DAWN SCHULTZ
715-737-2482 (OFFICE)
dawn.schultz@xcelenergy.com

WISCONSIN DNR - LIASON

DEPARTMENT OF NATURAL RESOURCES WEST CENTRAL REGION 1300 WEST CLAIREMONT AVE EAU CLAIRE WI 54701 ATTN: KAREN KALVELAGE (608) 785-9115

TREMPEALEAU COUNTY HIGHWAY DEPARTMENT

DAVE LYGA TREMPEALEAU COUNTY HIGHWAY COMMISSIONER LYGAD@TCC.COOP (715) 538-4799 EXT 27

GENERAL NOTES

EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO ANY GRUBBING OR TOPSOIL STRIPPING OPERATIONS.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO CALL DIGGERS HOTLINE PRIOR TO BEGINNING WORK OPERATIONS.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

DISTURBED AREAS WITHIN THE RIGHT OF WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE SALVAGE TOPSOILED, FERTILIZED, SEEDED, AND MULCHED AS DIRECTED BY THE ENGINEER.

A VERTICAL SAWCUT SHALL BE MADE THROUGH EXISTING PAVEMENTS AT REMOVAL LIMITS.

THE LOCATION OF DRIVEWAYS WILL BE DETERMINED BY THE ENGINEER.

ALL REINFORCED CONCRETE PIPE AND STORM SEWER PIPE SHALL HAVE THE END SECTIONS TIED TOGETHER. THE TIE SHALL CONSIST OF A MINIMUM OF 12-FEET OF PIPE PLUS AEW AND ARE INCIDENTAL TO THE BID ITEM.

WHEN THE QUANTITY OF ITEMS OF BASE AGGREGATE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE. THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER.

6.5—INCH HMA PAVEMENT SHALL BE CONSTRUCTED IN 3 LAYERS; 19MM 3—INCH (BOTTOM), 12.5MM 1.75—INCH (MIDDLE), AND 12.5MM 1.75—INCH (UPPER). THE SHOULDER SHALL BE PLACED CONCURRENT WITH THE MAINLINE PAVEMENT. PAVING SHALL BE CONTINUOUS THROUGH THE CULVERT REPLACEMENT AREAS.

4—INCH HMA PAVEMENT SHALL BE CONSTRUCTED IN 2 LAYERS; 19MM 2—INCH (BOTTOM-LEVELING LAYER), 12.5MM 2—INCH (UPPER). NOTE THE BOTTOM LAYER IS A LEVELING LAYER SO DENSITY TESTS ARE NOT REQUIRED.

FROM STATION 38+10 TO STATION 87+97 THE BOTTOM 3-INCH LIFT SHALL BE PAVED WITHIN 24 HOURS OF MILLING. THE REMAINING LIFTS NEED TO BE PAVED WITHIN 10 DAYS OF PAVING THE FIRST LIFT.

FROM STATION 87+97 TO 118+20 TRAFFIC SHALL NOT DRIVE ON THE MILLED SURFACE FOR MORE THAN 10 DAYS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS ARE APPROXIMATE AND MAY BE ADJUSTED TO FIT FIELD CONDITIONS

THE CONTRACTOR WILL BE RESPONSIBLE FOR CUTTING WEEP HOLES AFTER MILLING TO ASSURE THE ROAD WILL DRAIN. THIS IS INCIDENTAL TO THE ITEM OF REMOVING ASPHALTIC SURFACE MILLINGS.

THE CONTRACTOR WILL BE RESPONSIBLE FOR GRADING BACK THE SHOULDERS AFTER MILLING TO REMOVE THE VERTICAL EDGE. THIS IS INCIDENTAL TO THE ITEM OF SHAPING SHOULDERS.

THE LATEST APPROVED DOT LONGITUDINAL HMA WEDGE JOINT SHOULD NOT BE USED. IF USED THE CONTRACTOR WILL BE REQUIRED TO MILL OUT ANY WEDGE USED FOR TRAFFIC STAGING PRIOR TO PLACING THE ADJACENT LANE AT NO COST TO THE DEPARTMENT

THE STORM SEWER PIPE, CONCRETE, AND HMA PAVEMENT AT STATION 36+70 SHALL BE REPLACED WHILE TRAFFIC IS BEING DETOURED AND CONCURRENTLY WITH THE CULVERT REPLACEMENT AT STATION 39+00.

THE CONCRETE PAVEMENT AND CONCRETE CENTERLINE REPAIRS SHALL BE COMPLETED WHILE TRAFFIC IS BEING DETOURED.

WISCONSIN DOT

DEPARTMENT OF TRANSPORTATION NORTH WEST REGION 718 WEST CLAIREMONT AVE EAU CLAIRE WI 54701 ATTN.: TIM MASON (715) 833-5366



PROJECT NO:1520-04-71

U.G.

V.C.

HWY: USH 10

COUNTY: TREMPEALEAU

GENERAL NOTES, UTILITIES, ABBREVIATIONS

SHEET

PLOT SCALE . 0 00/4518

FILE NIVINE . C./ MICHOL/ DECICN/ 12300/01/ CHEELCH VVI/ USU101 UN 1167 10 DM

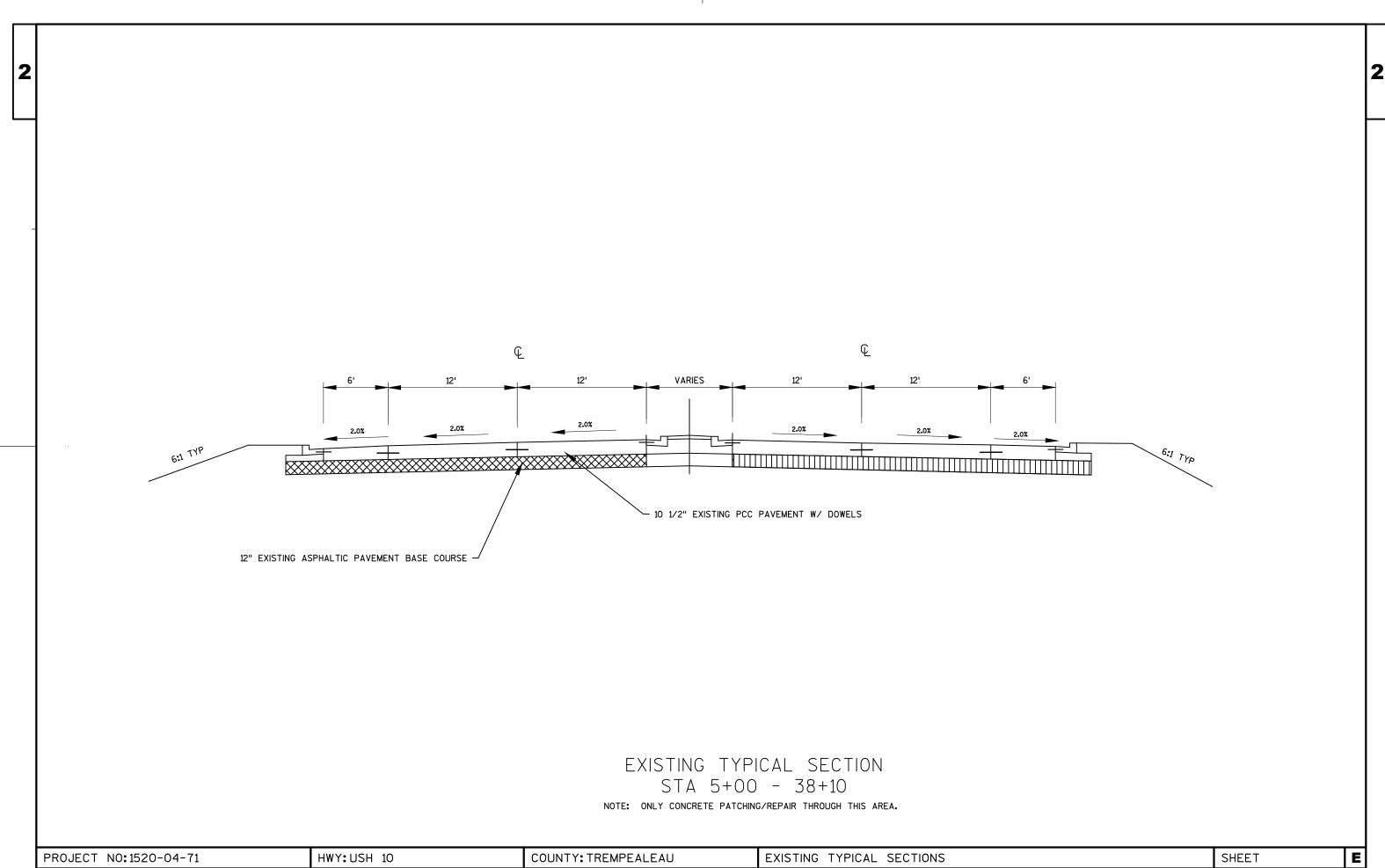
UNDERGROUND

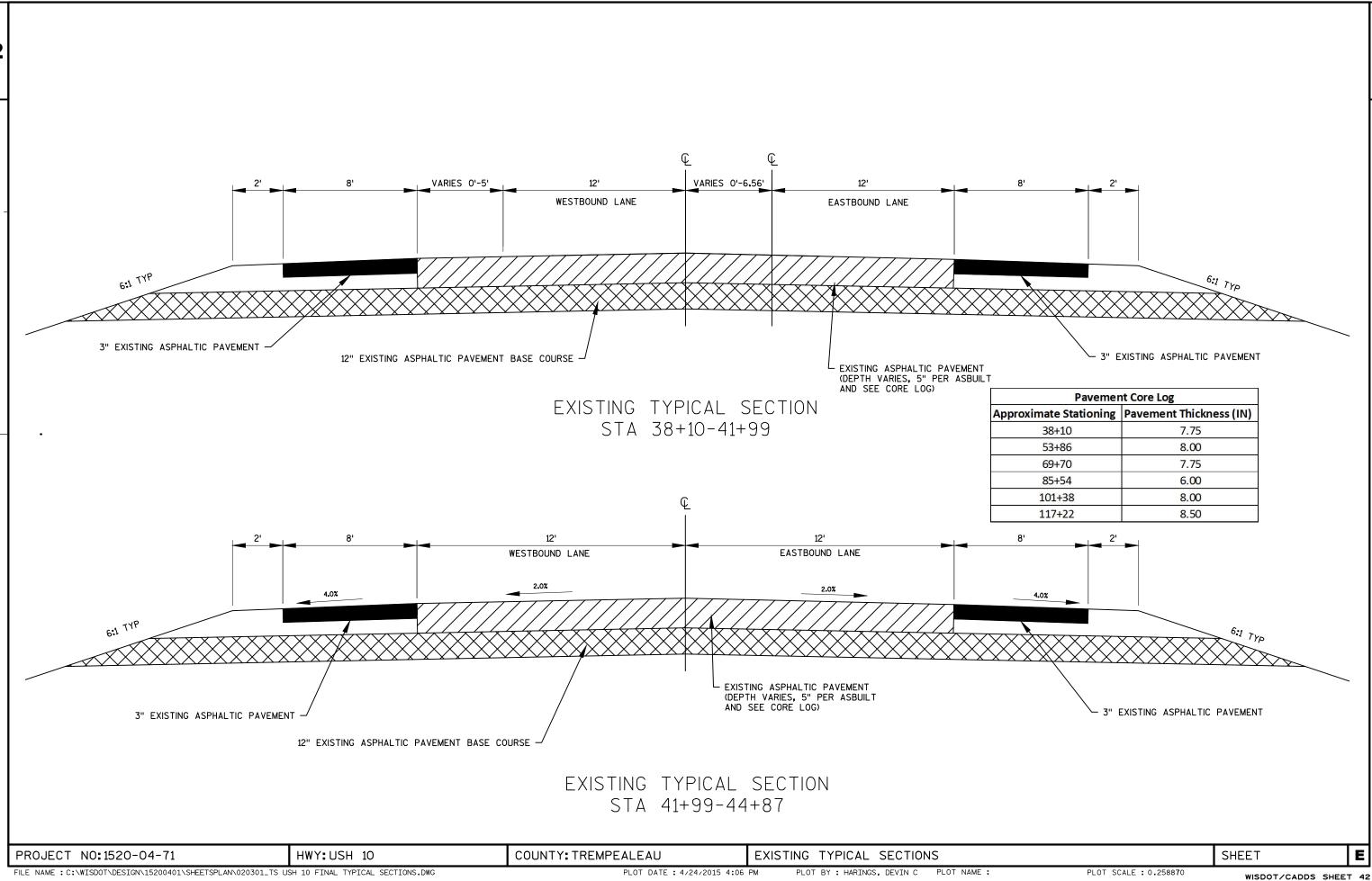
VERTICAL CURVE

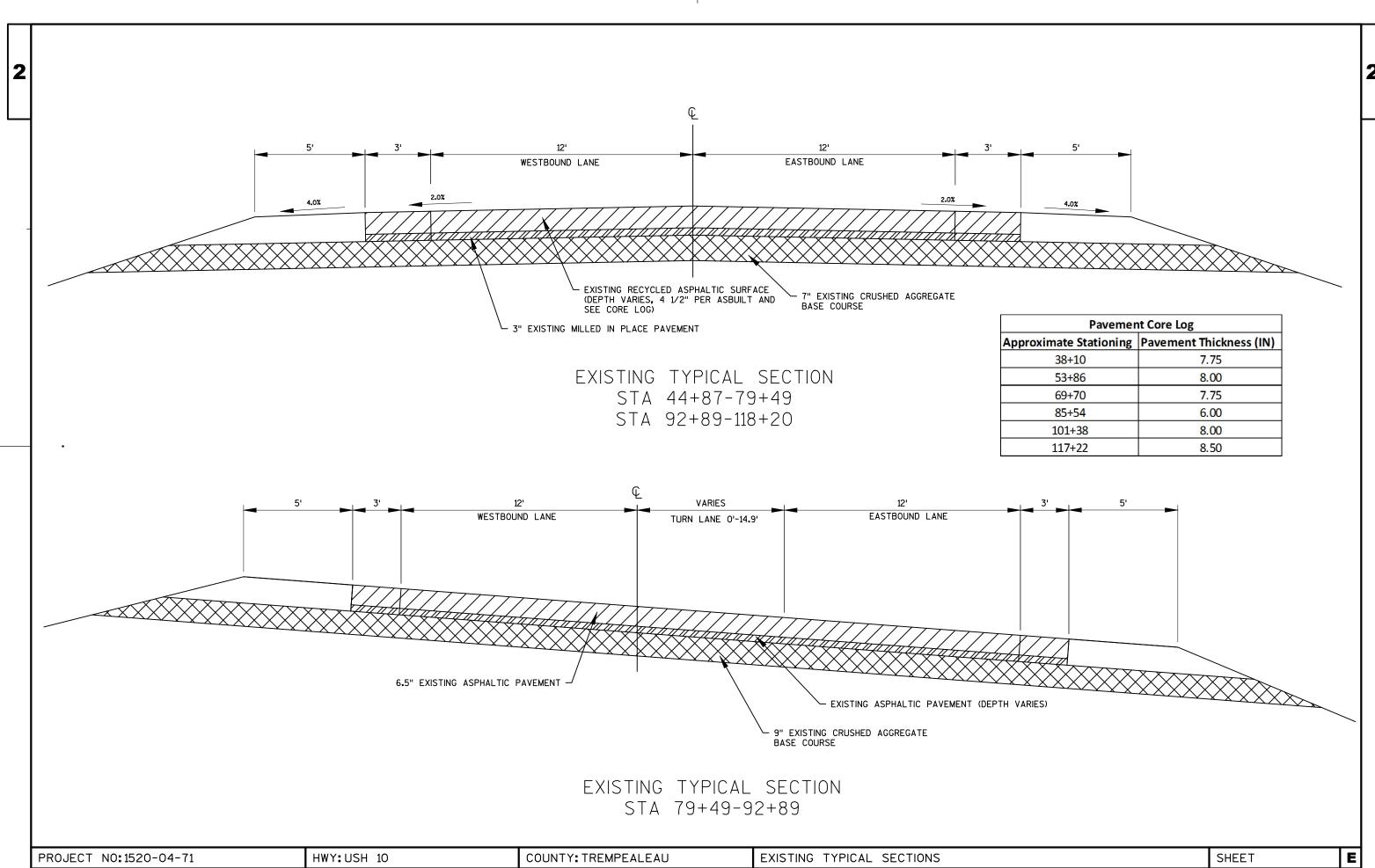
VELOCITY OR DESIGN SPEED

DIOT DATE . Q.Z. 2008 Q.ZE AM

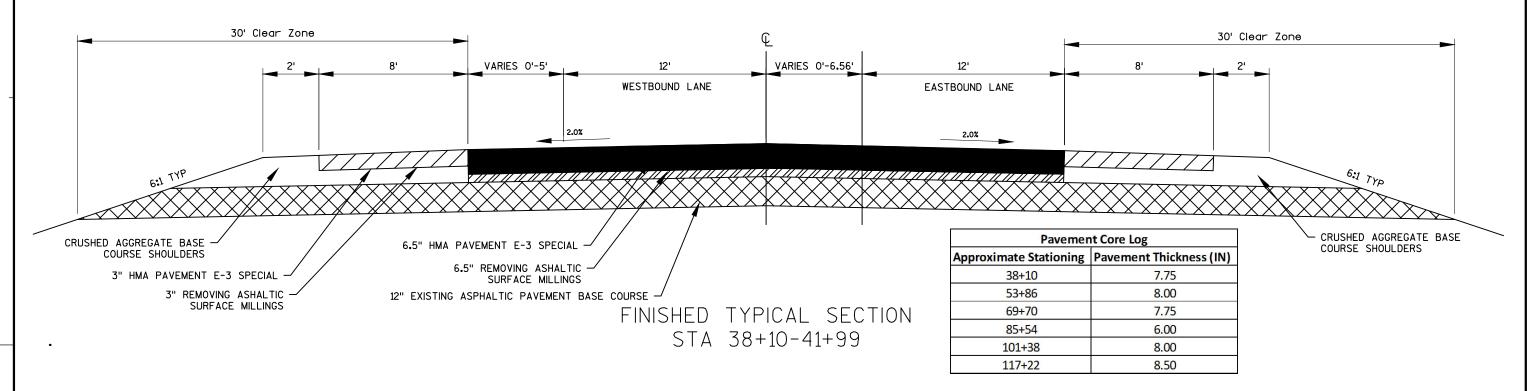
DIOT BY - HADTNICE DEVINIC PLOT NAME

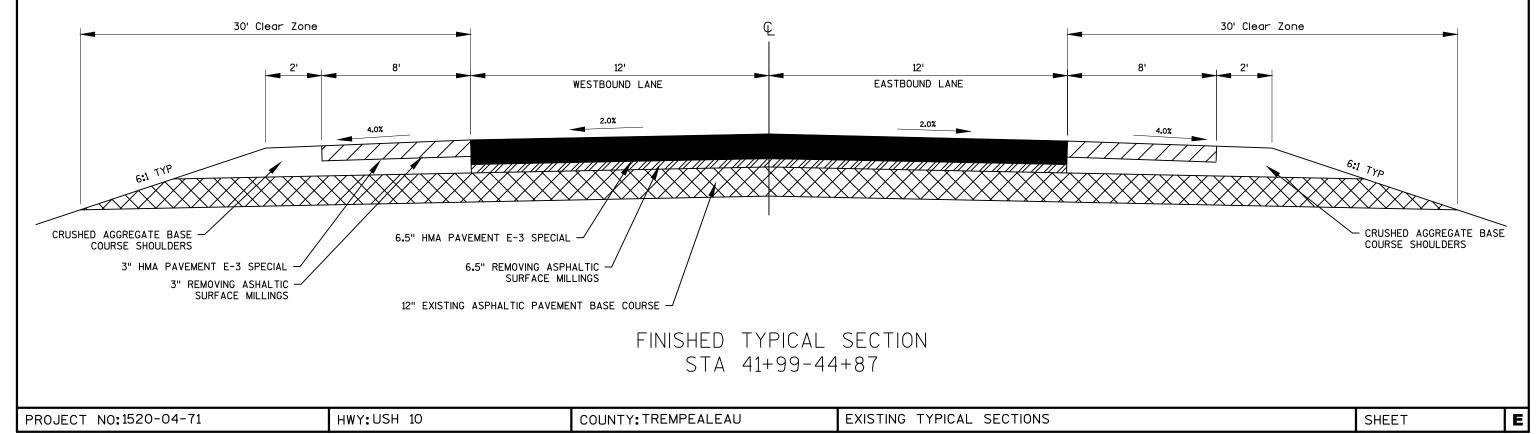


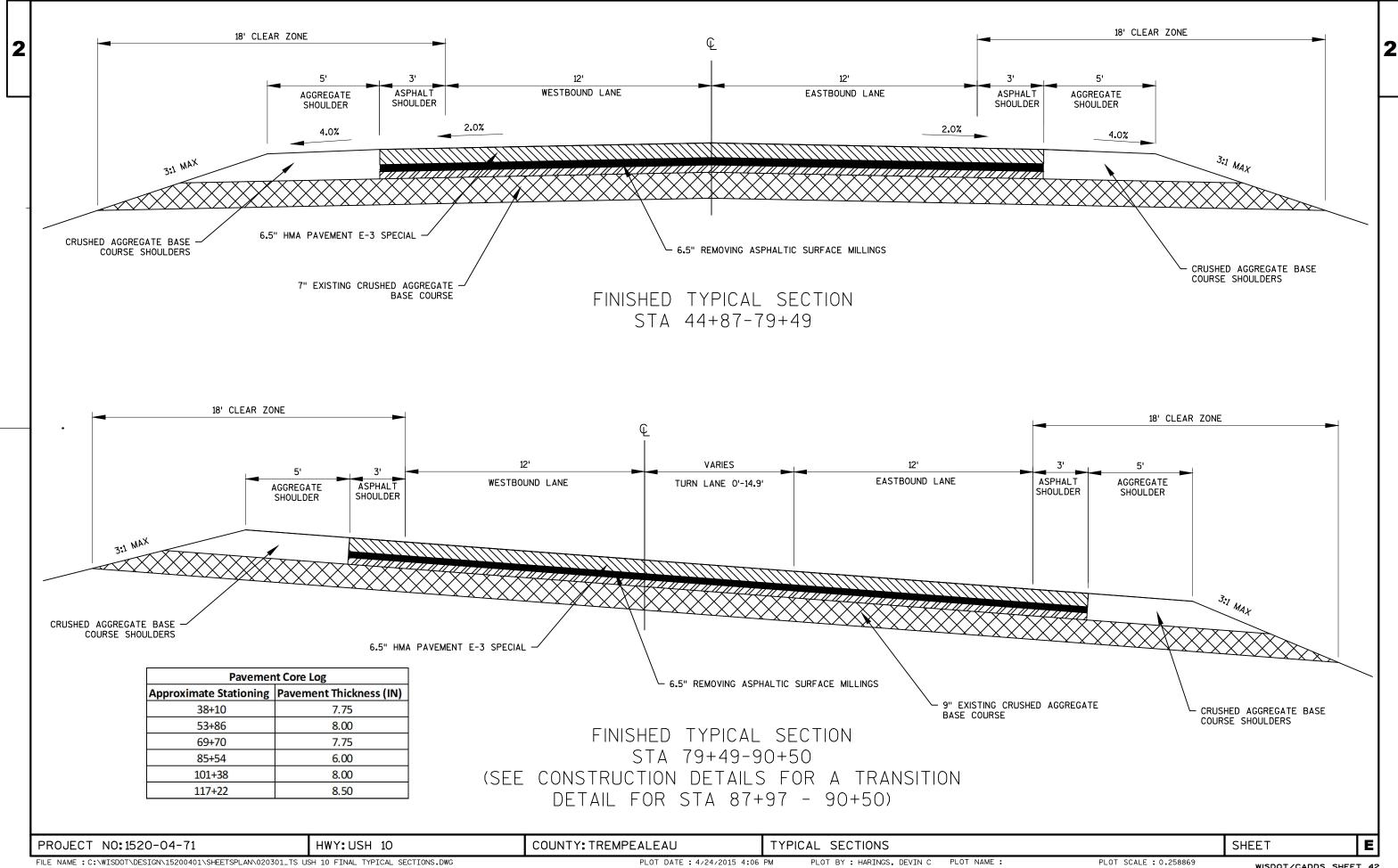


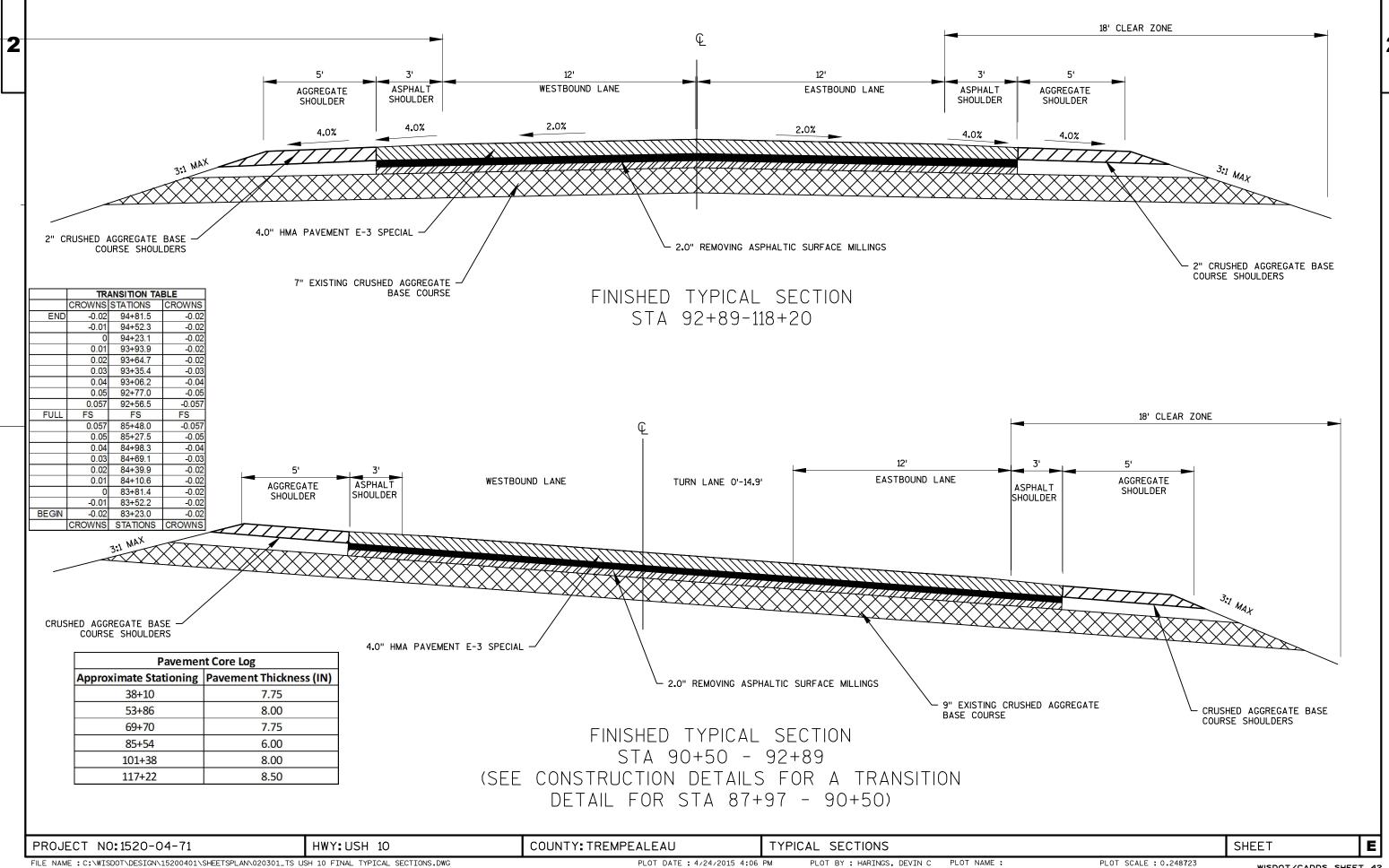




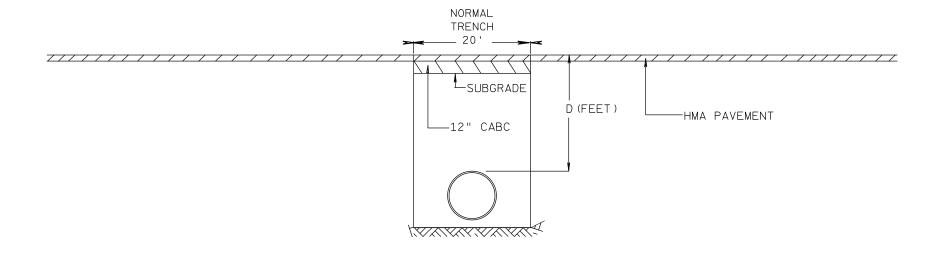


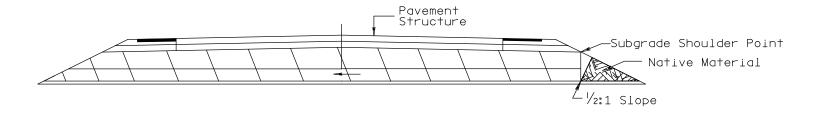








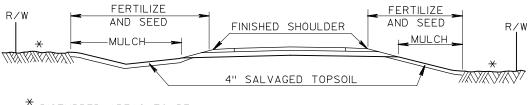




CROSS DRAIN INSTALLATION DETAIL

TRENCH BOX REQUIRED. INCIDENTAL TO CULVERT PIPE ITEM

CABC AND PAVEMENT ITEMS ARE PAID FOR UNDER ITEMS IN THE CONTRACT

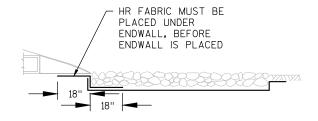


* DISTURBED AREAS TO BE FERTILIZED & SEEDED

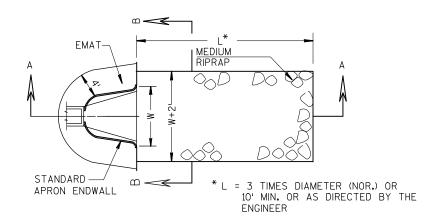
DETAIL FOR FINISHING ITEMS

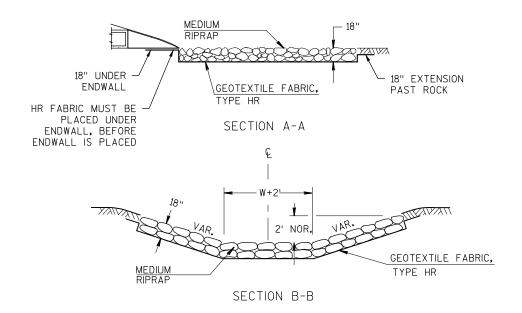
PROJECT NO:1520-04-71 HWY:USH 10 COUNTY:TREMPEALEAU PLAN: CONSTRUCTION DETAILS SHEET E

HR FABRIC MAY BE INSTALLED AS TWO SEPARATE PIECES, OVERLAPPING AS SHOWN OR AS APPROVED BY THE ENGINEER IN THE FIELD. EXTRA QUANTITY FOR OVERLAP IS INCIDENTAL TO THE CONTRACT.

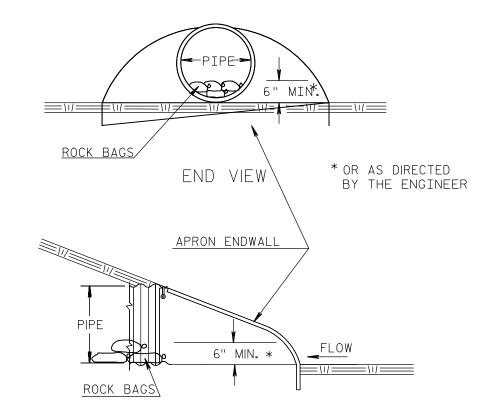


ALTERNATE HR FABRIC INSTALLATION





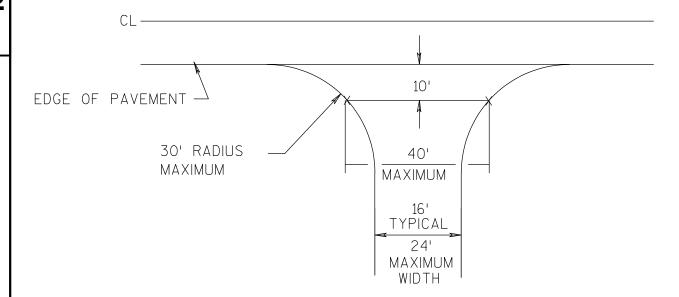
MEDIUM RIPRAP AND GEOTEXTILE FABRIC DETAIL AT APRON ENDWALLS



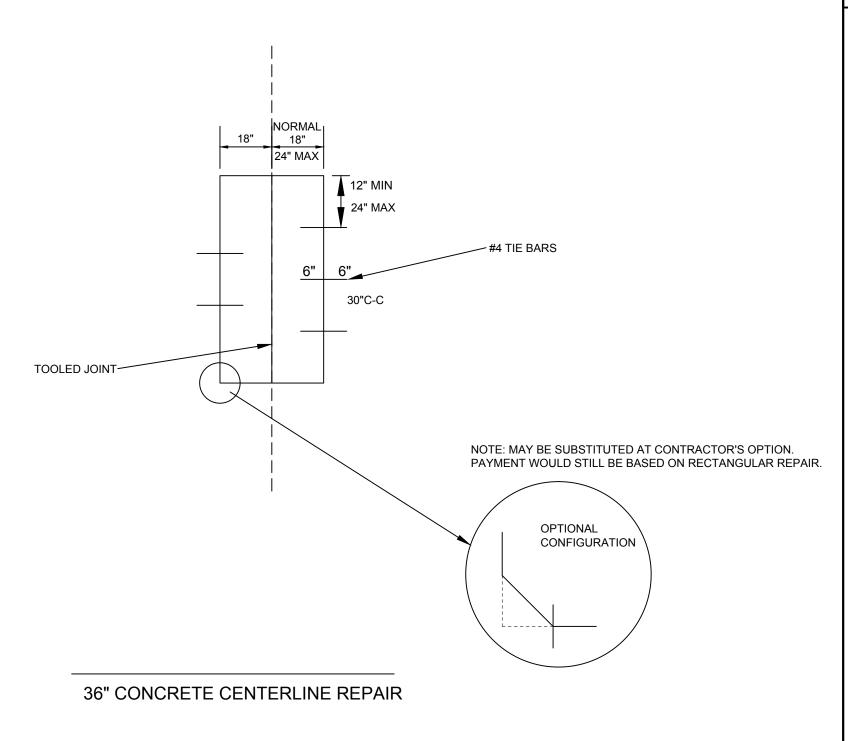
SIDE VIEW

CULVERT PIPE CHECK

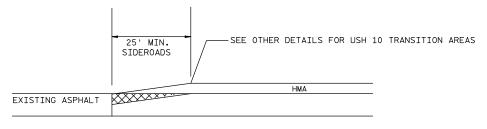
PROJECT NO:1520-04-71 HWY:USH 10 COUNTY:TREMPEALEAU PLAN: CONSTRUCTION DETAILS SHEET



TYPICAL DRIVEWAY DETAIL (NON-COMMERCIAL RURAL)



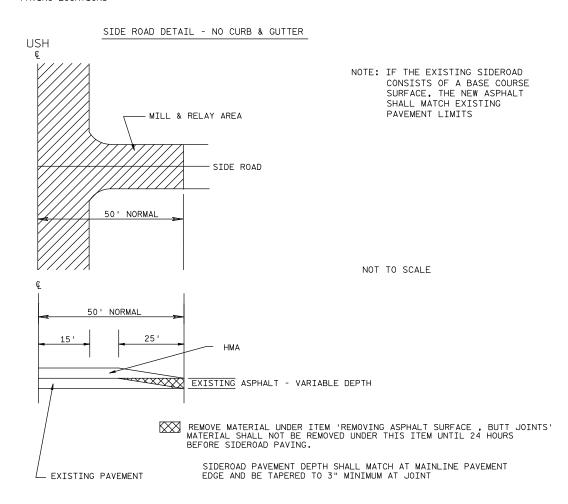
2



BUTT JOINT FOR SIDE ROADS

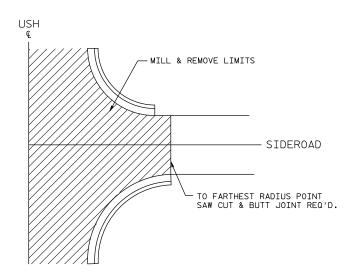
PAID FOR AS REMOVING ASPHALTIC SURFACE BUTT JOINTS

REQUIRED AT BEGIN AND END PAVING LOCATIONS



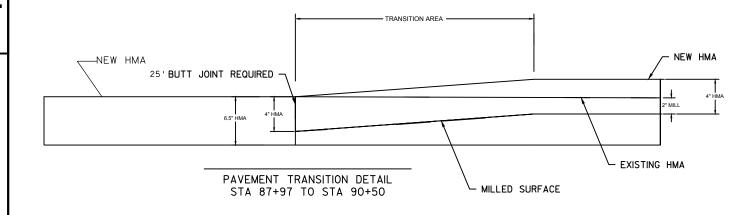
SIDEROAD DETAIL - CURB & GUTTER TO REMAIN

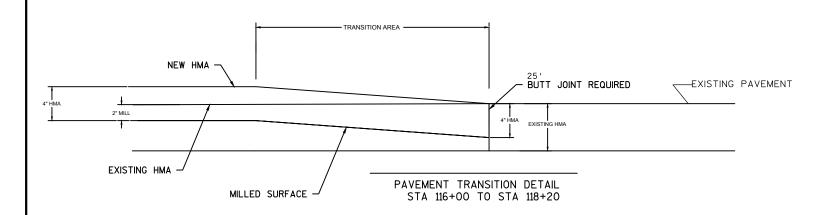
NOT TO SCALE

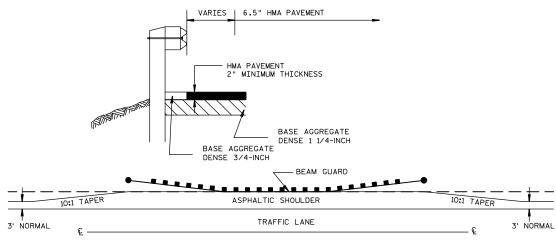


PROJECT NO:1520-04-71 HWY:USH 10 COUNTY:TREMPEALEAU PLAN: CONSTRUCTION DETAILS SHEET **E**



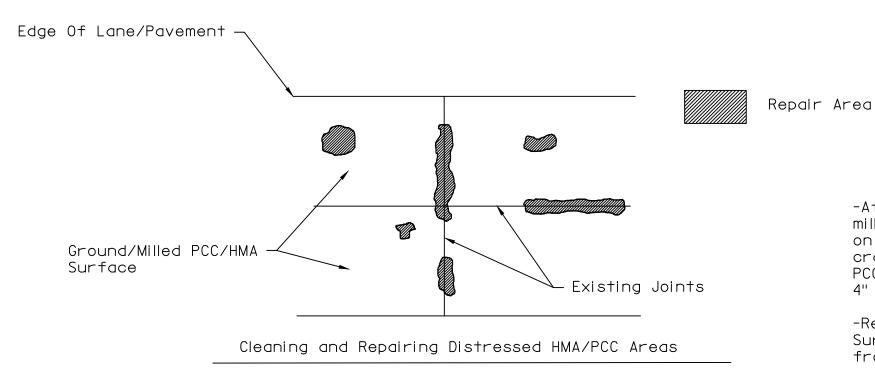






DETAIL FOR ASPHALTIC SHOULDER AT BEAM GUARD

2



<u>Notes</u>

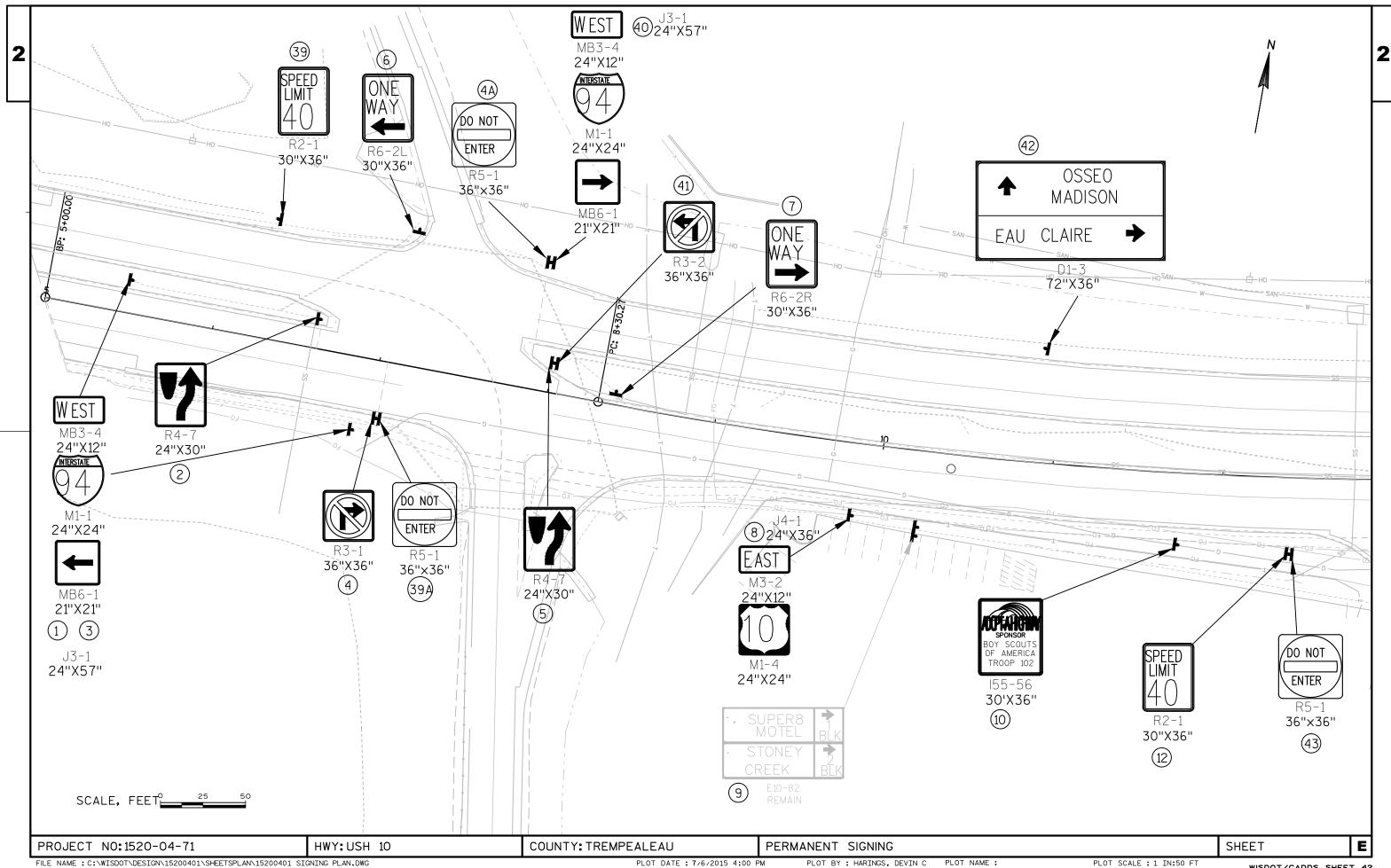
-After the existing pavement is milled/ground to depth specified on typical, remove remainder of crackfill, patching, and unsound PCC/HMA to a minimum depth of 4" below the pavement surface.

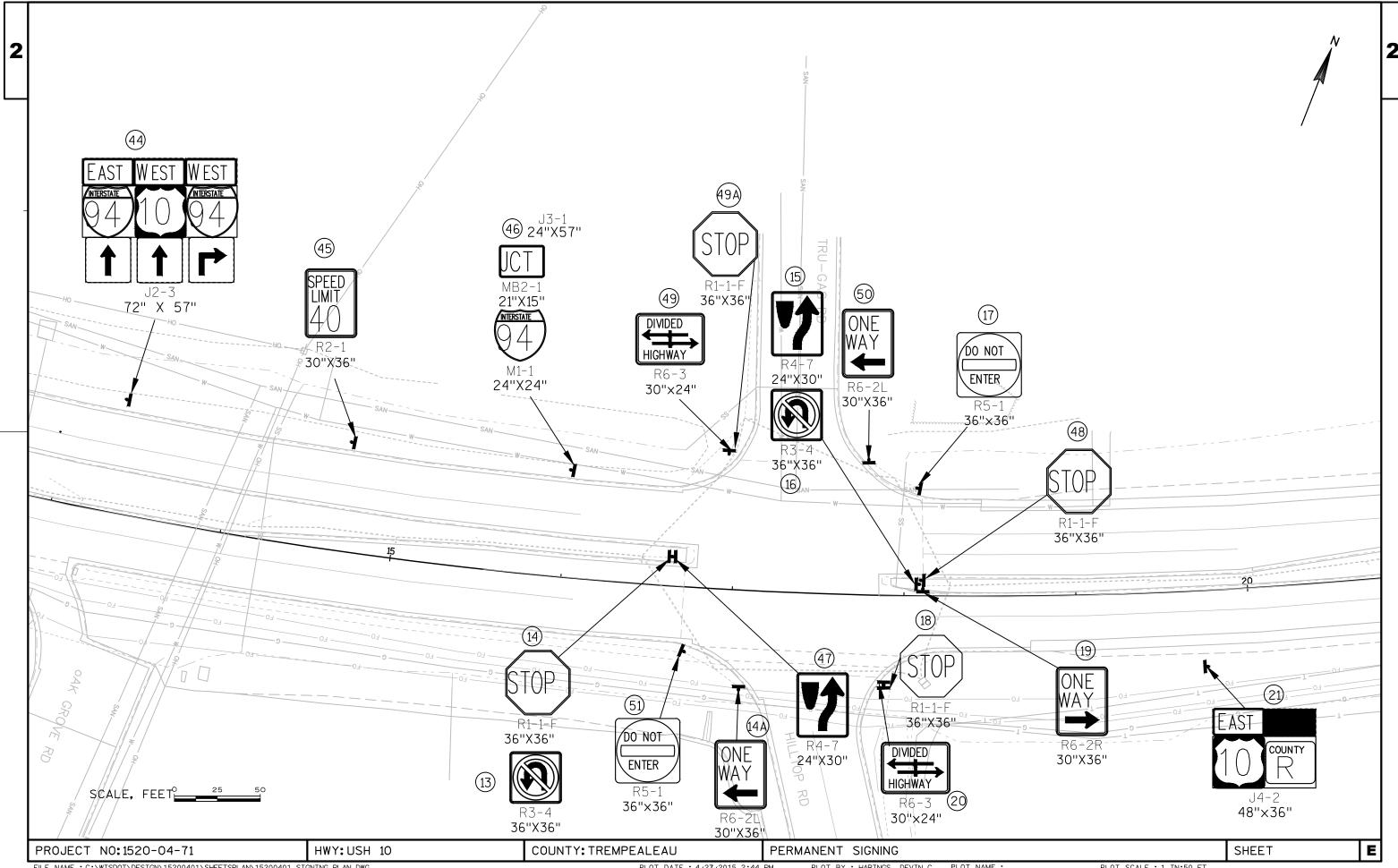
-Repave areas with Asphaltic Surface Patching paid separately from this item.

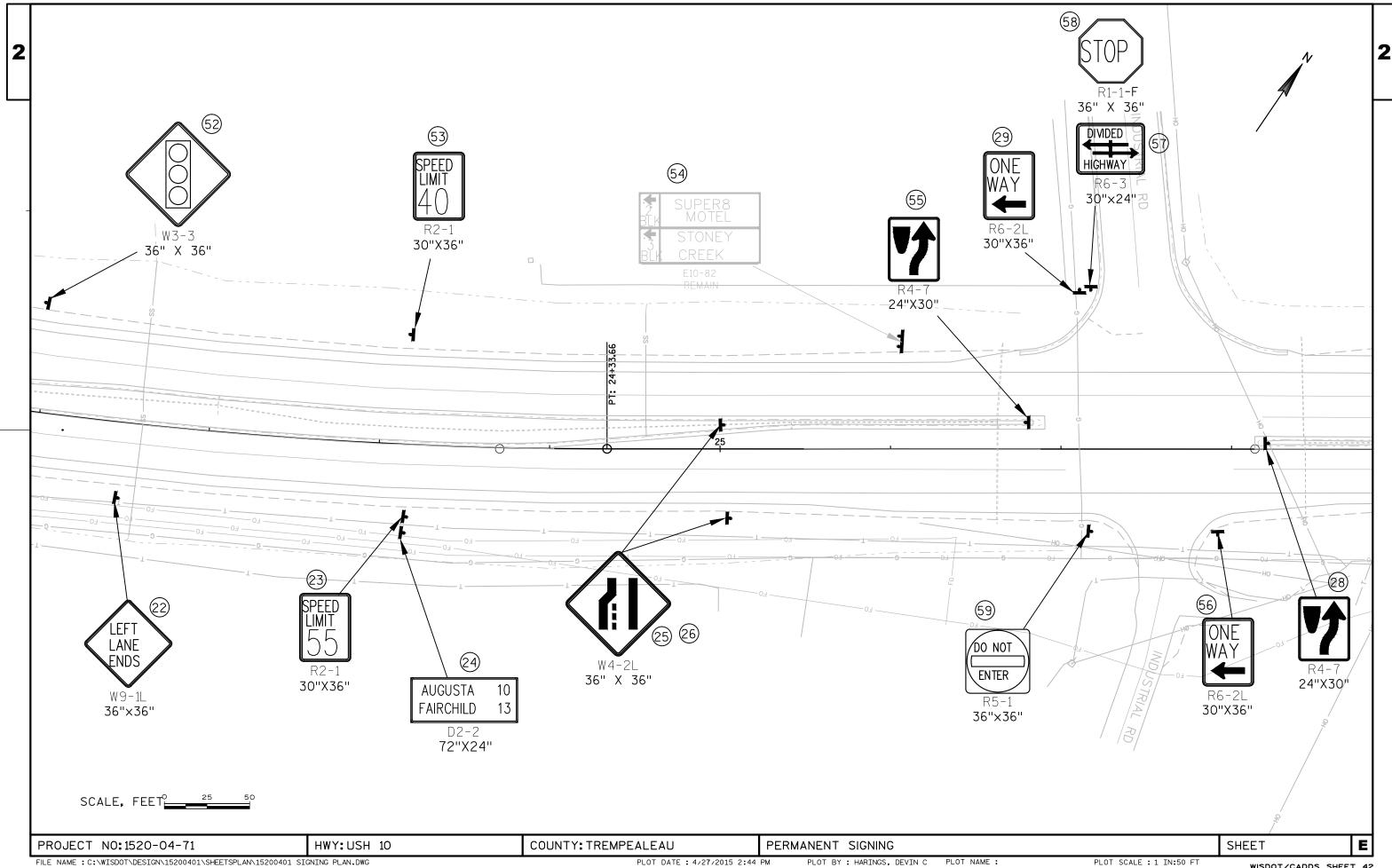
PREPARATION OF FOUNDATION FOR ASPHALTIC PAVING SPECIAL

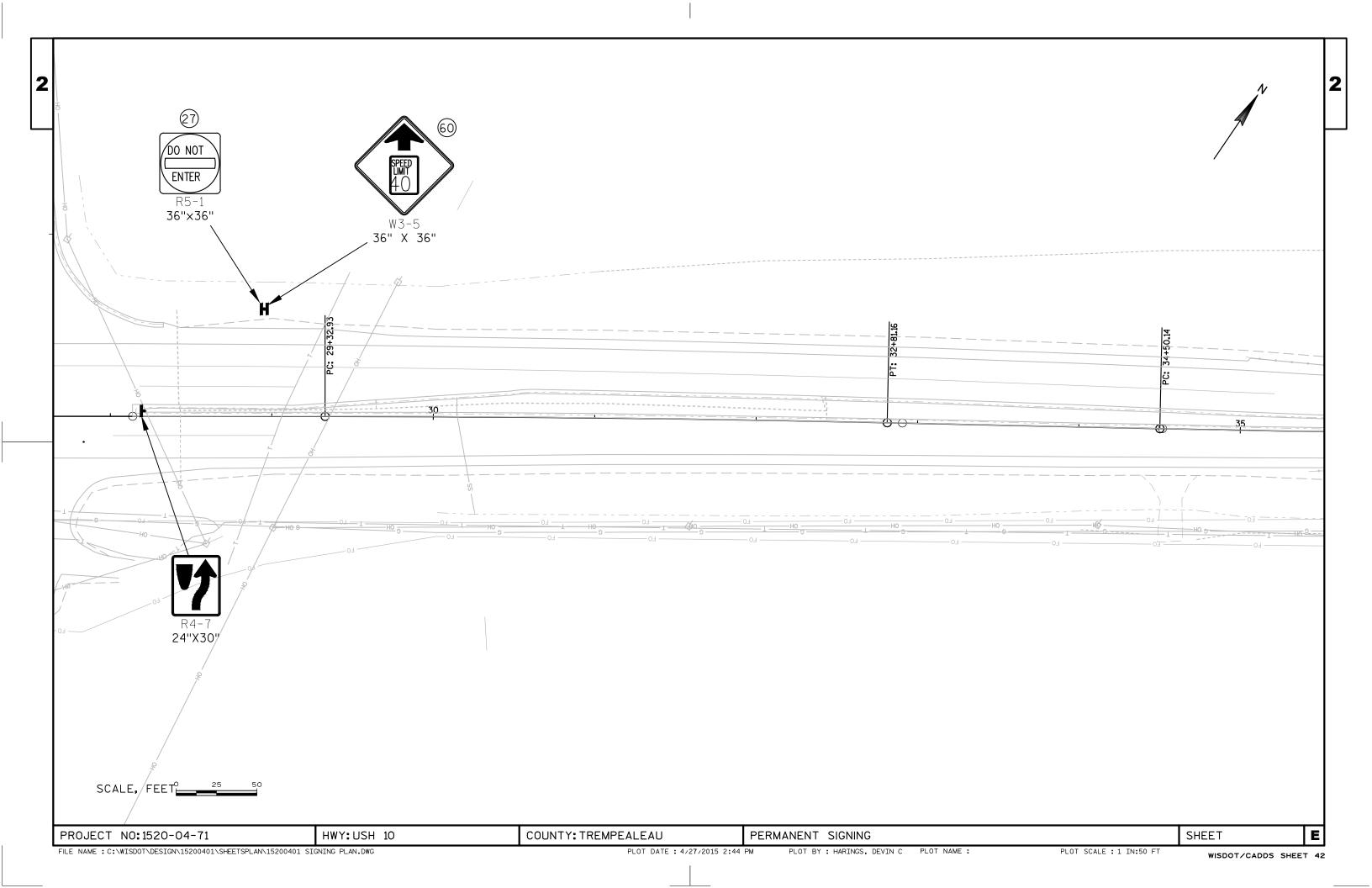
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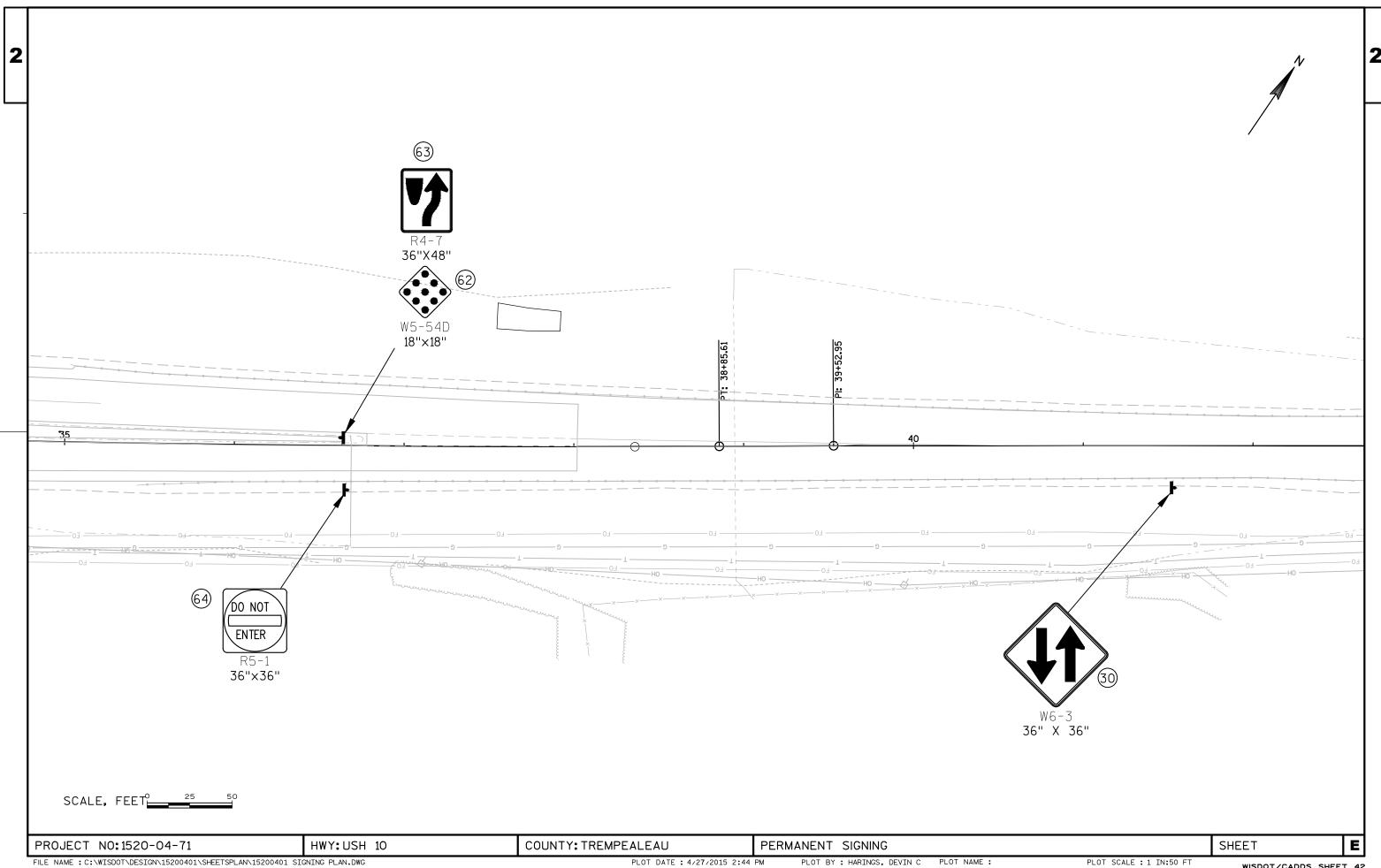
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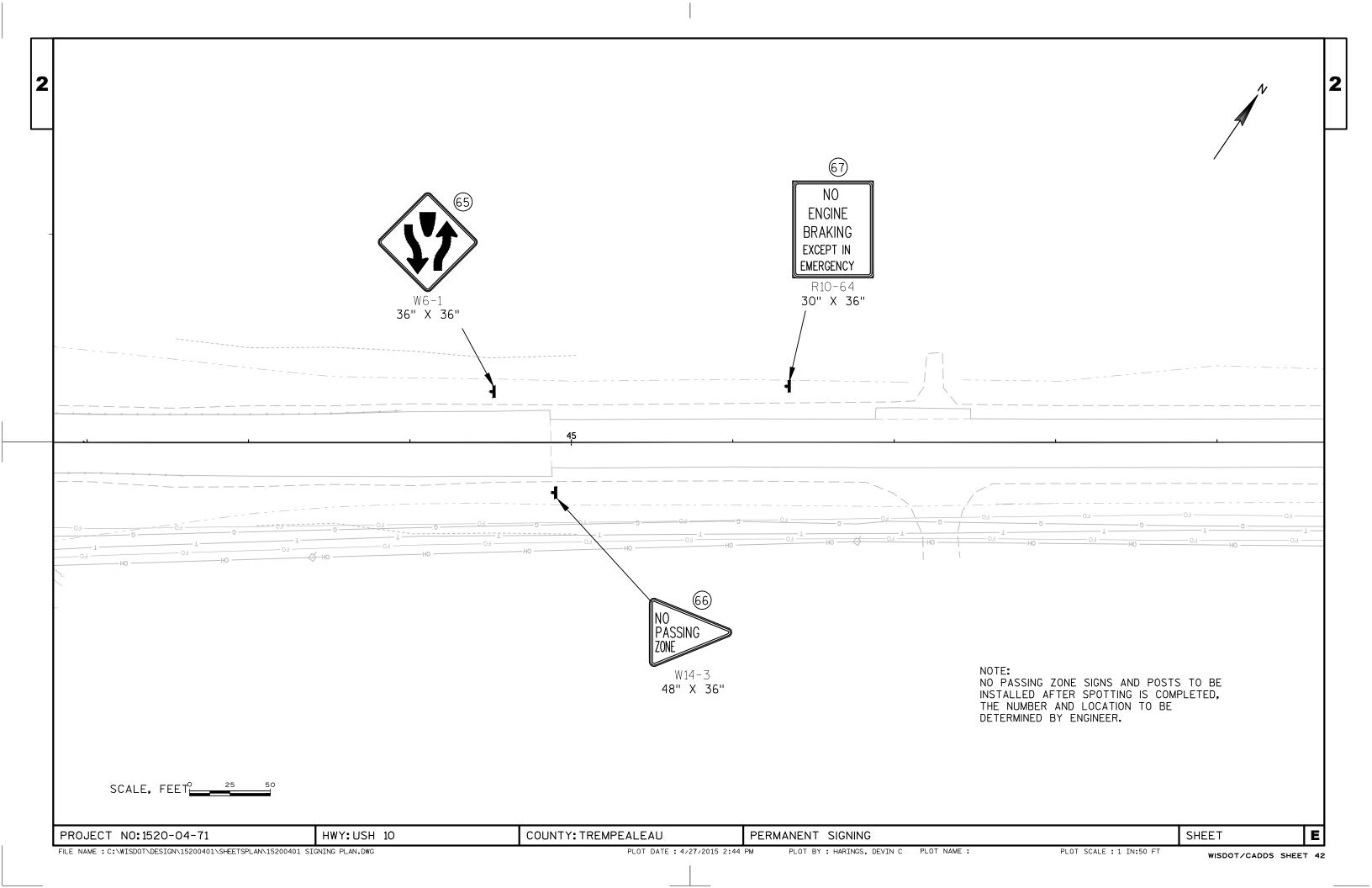


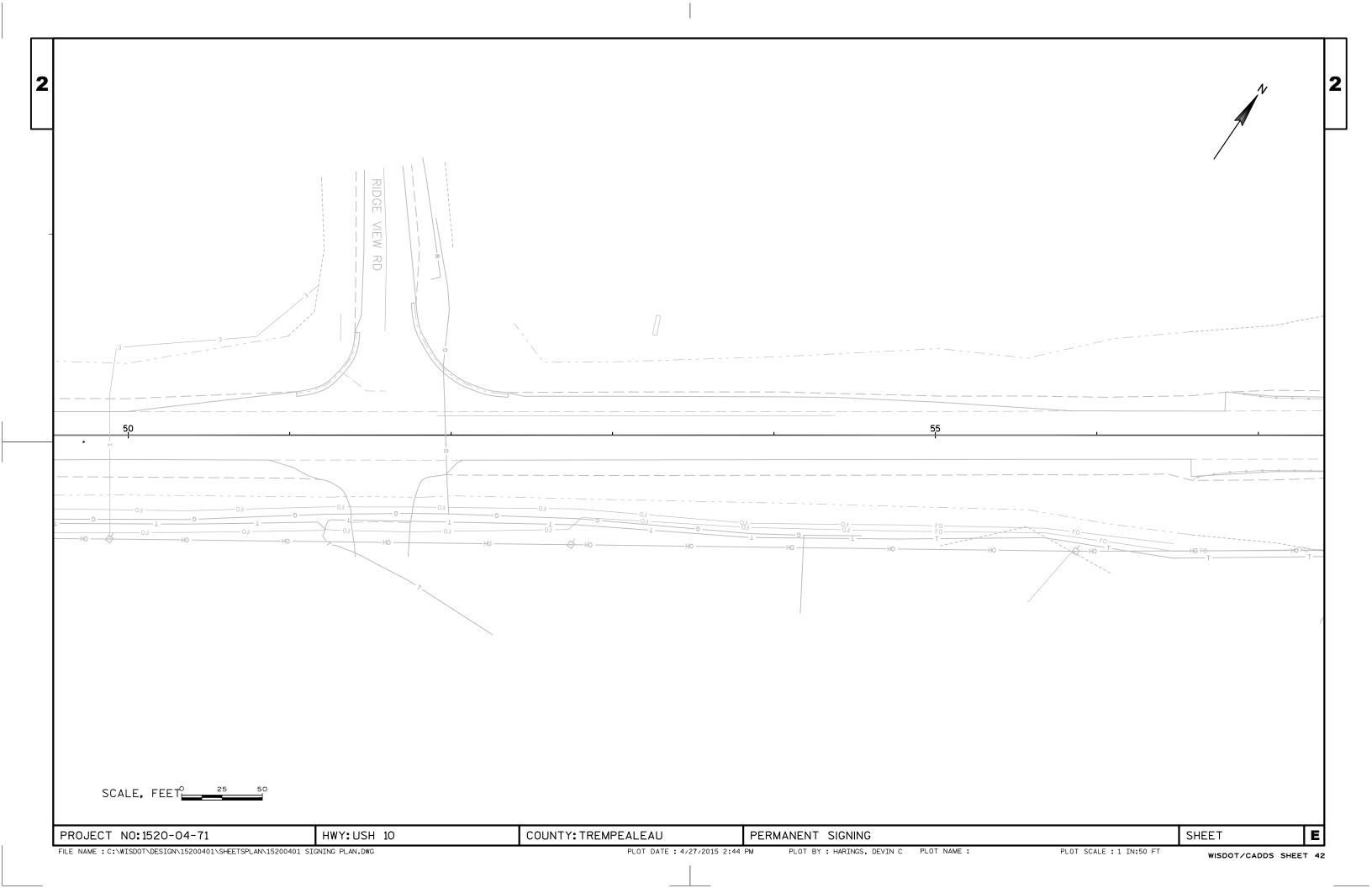


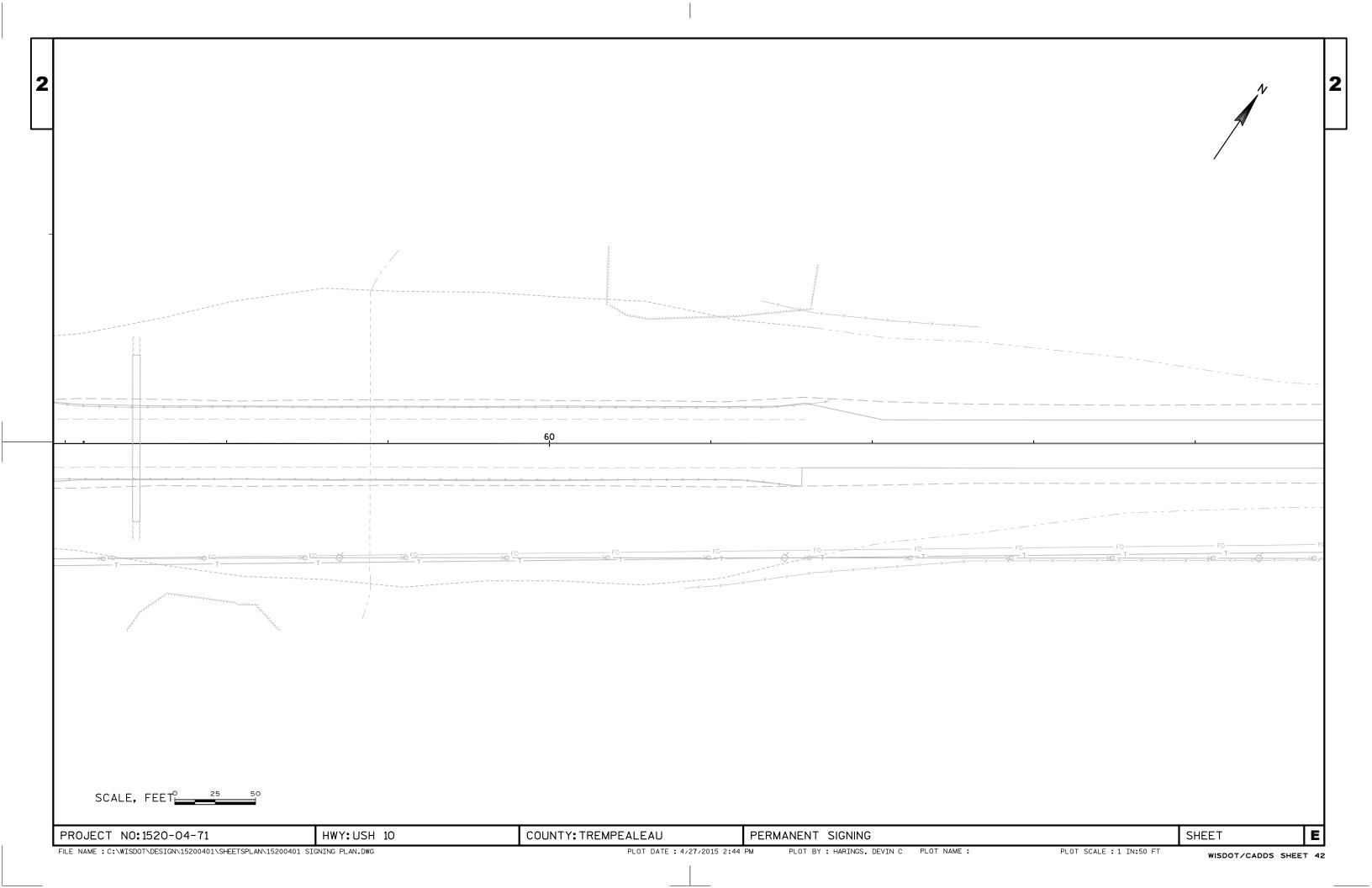


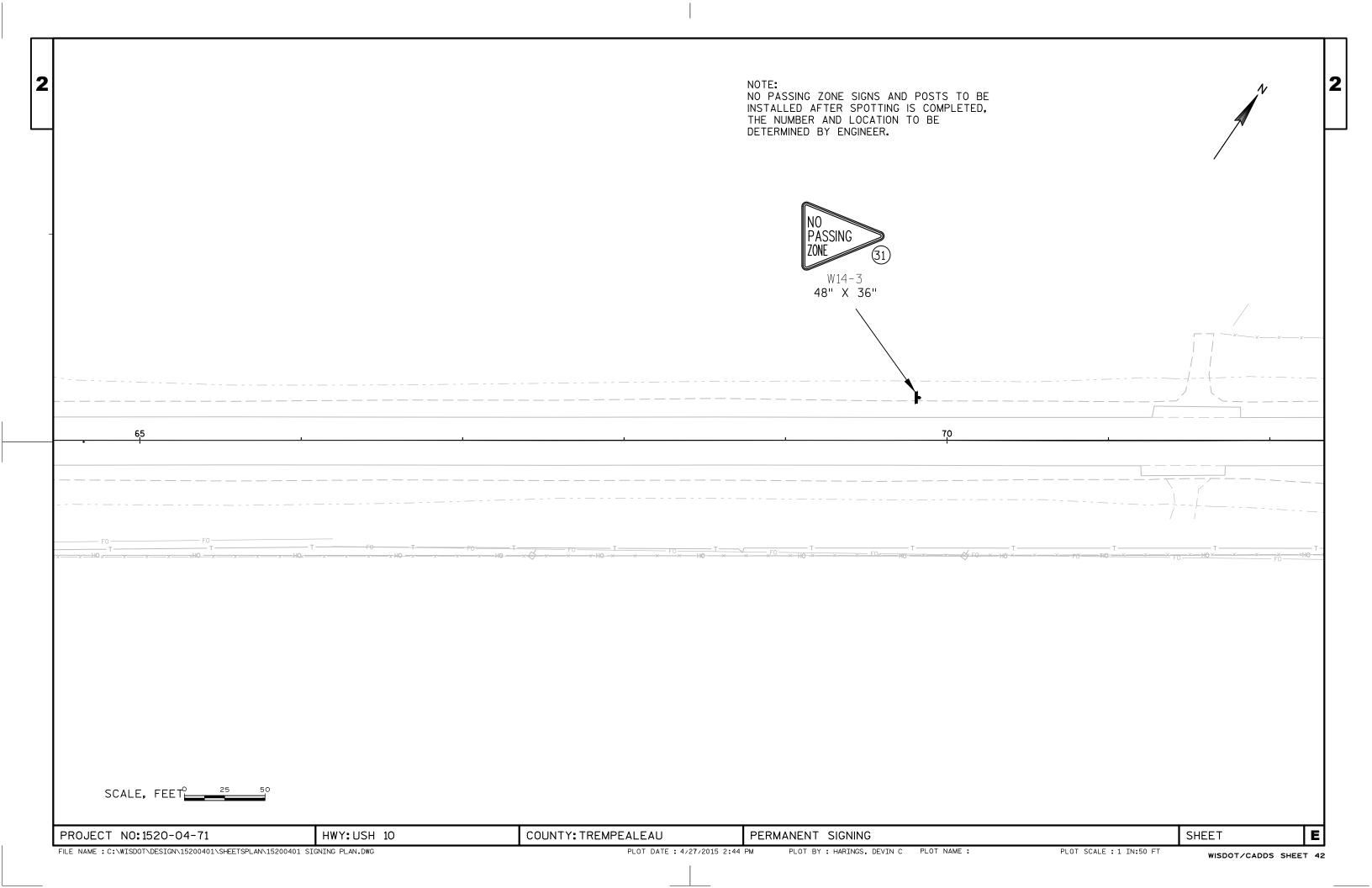


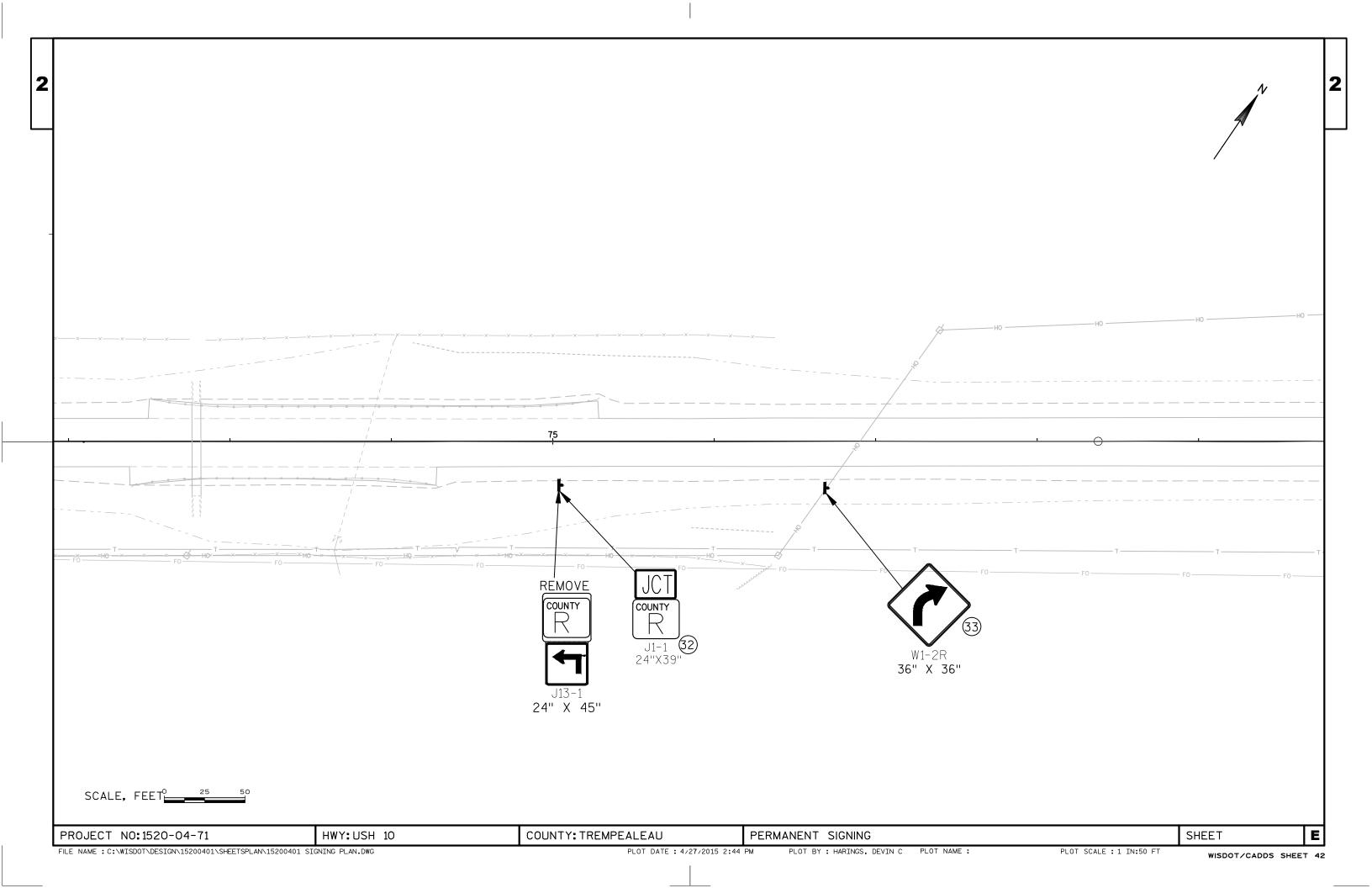


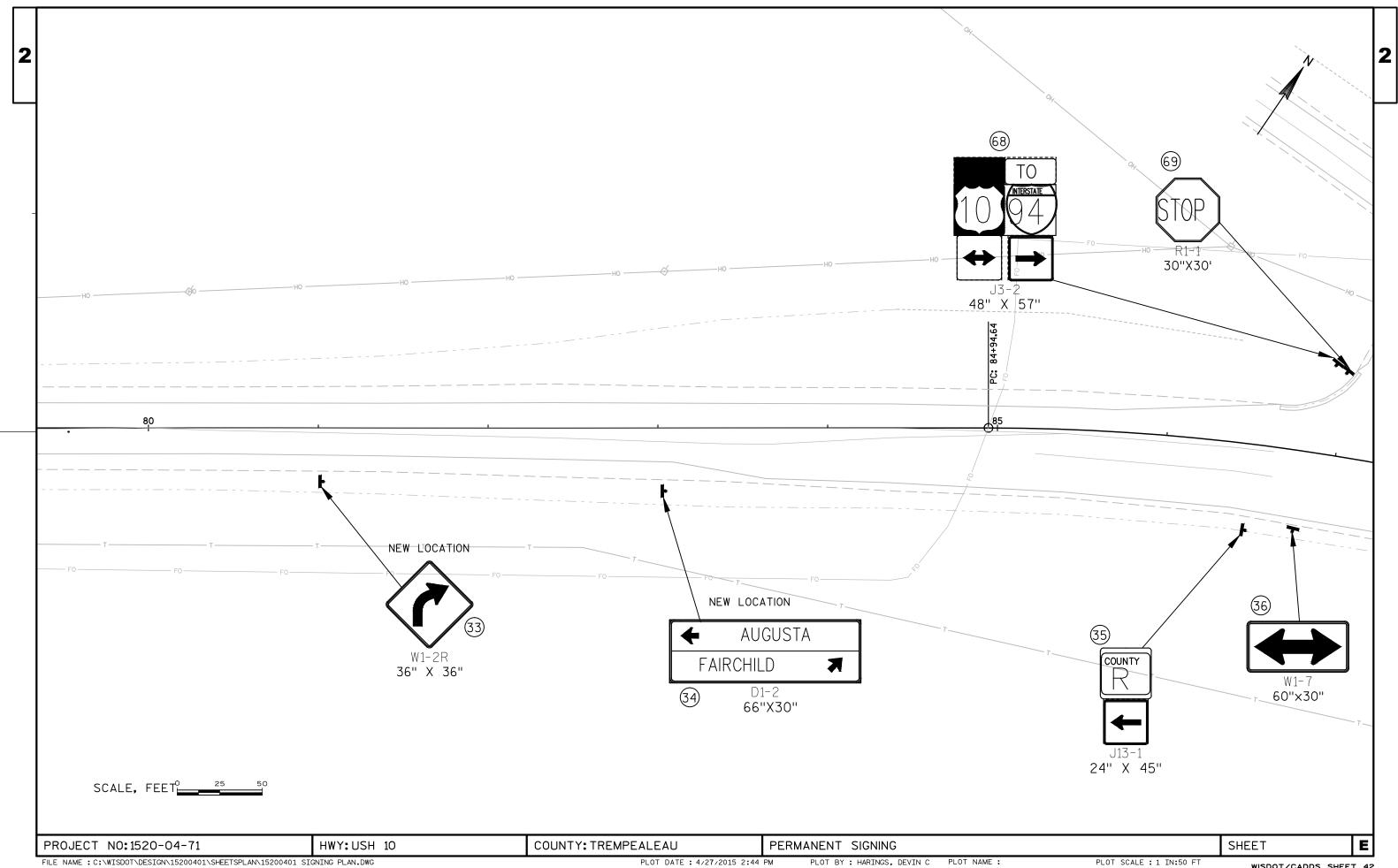


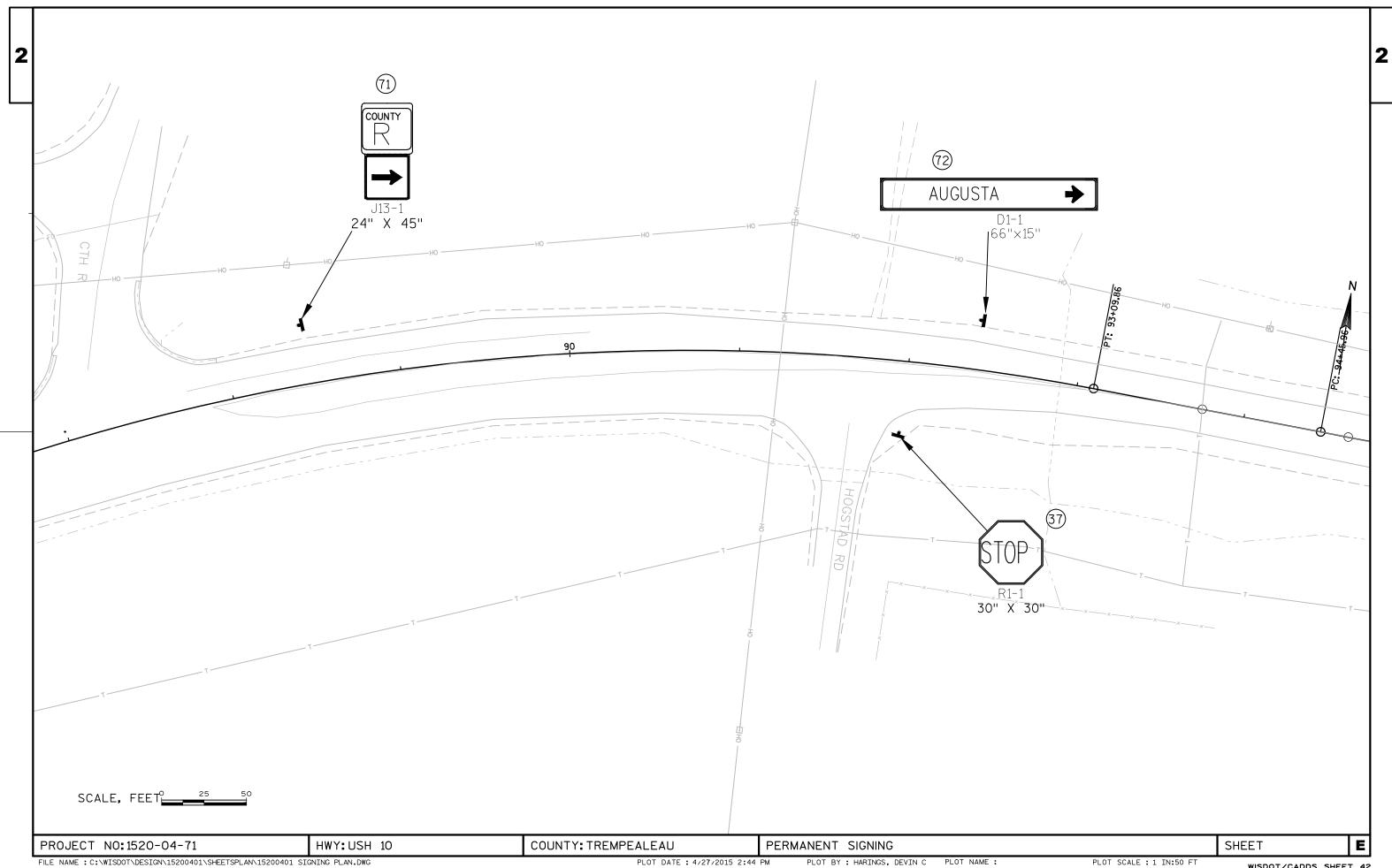


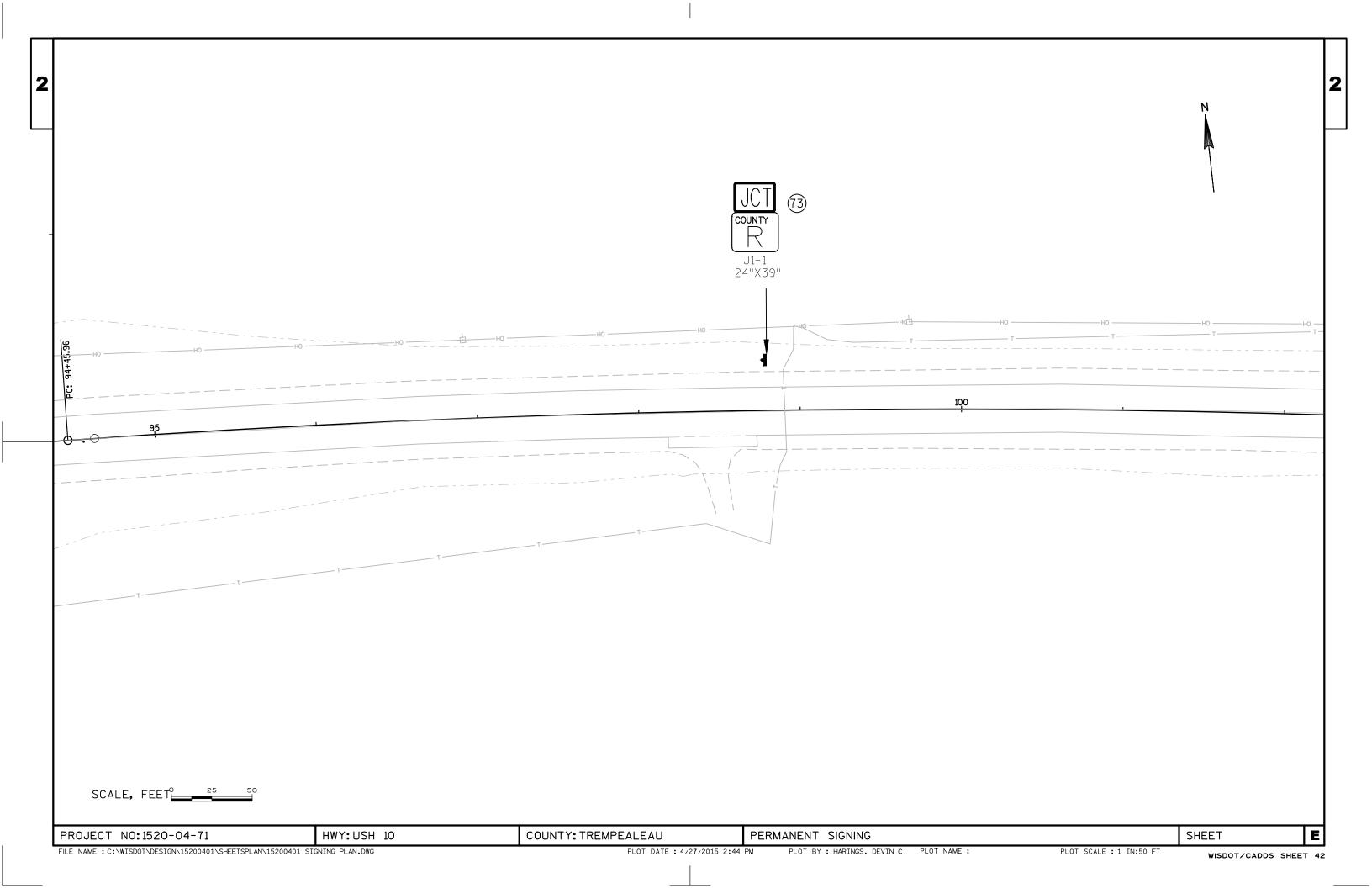


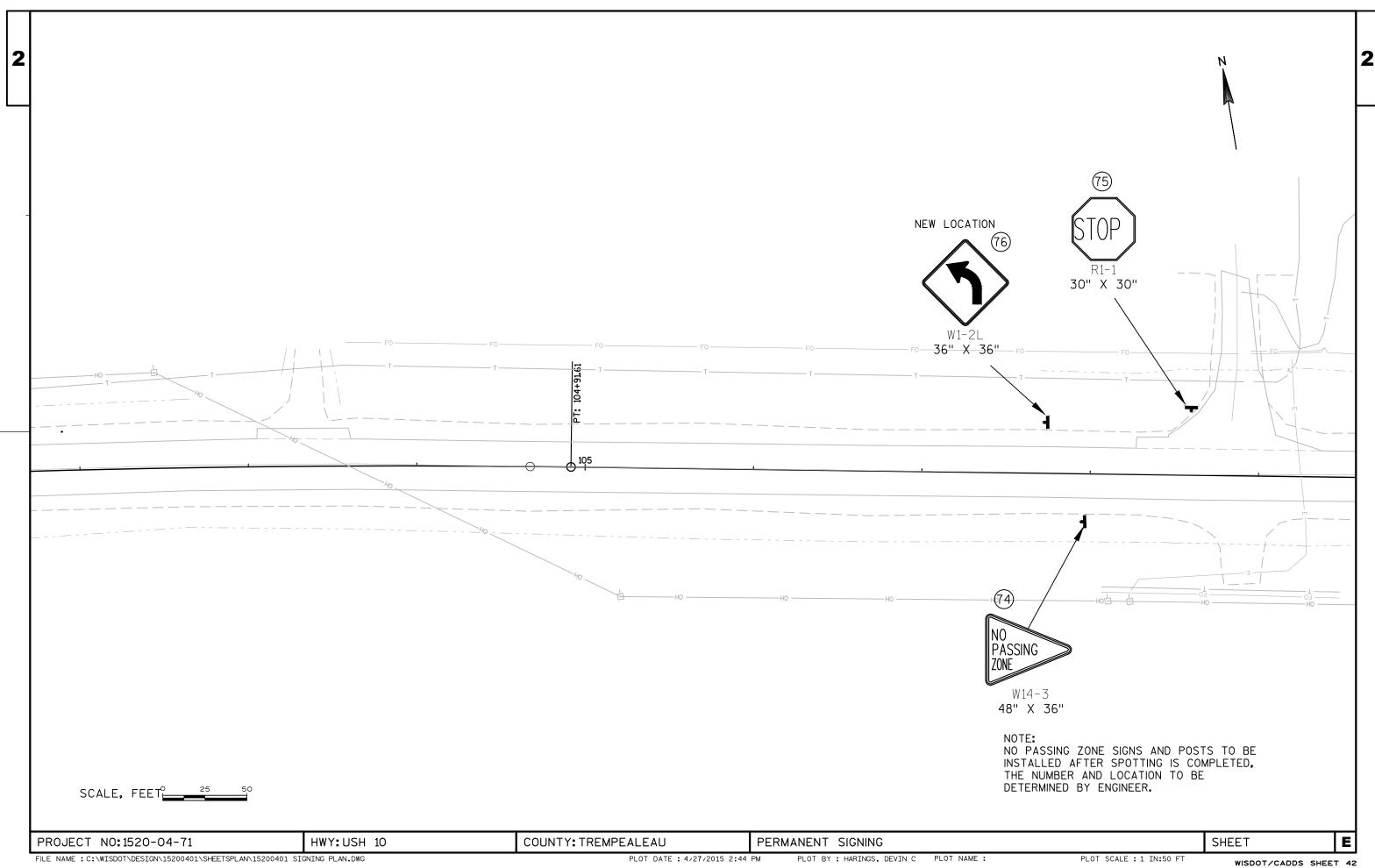


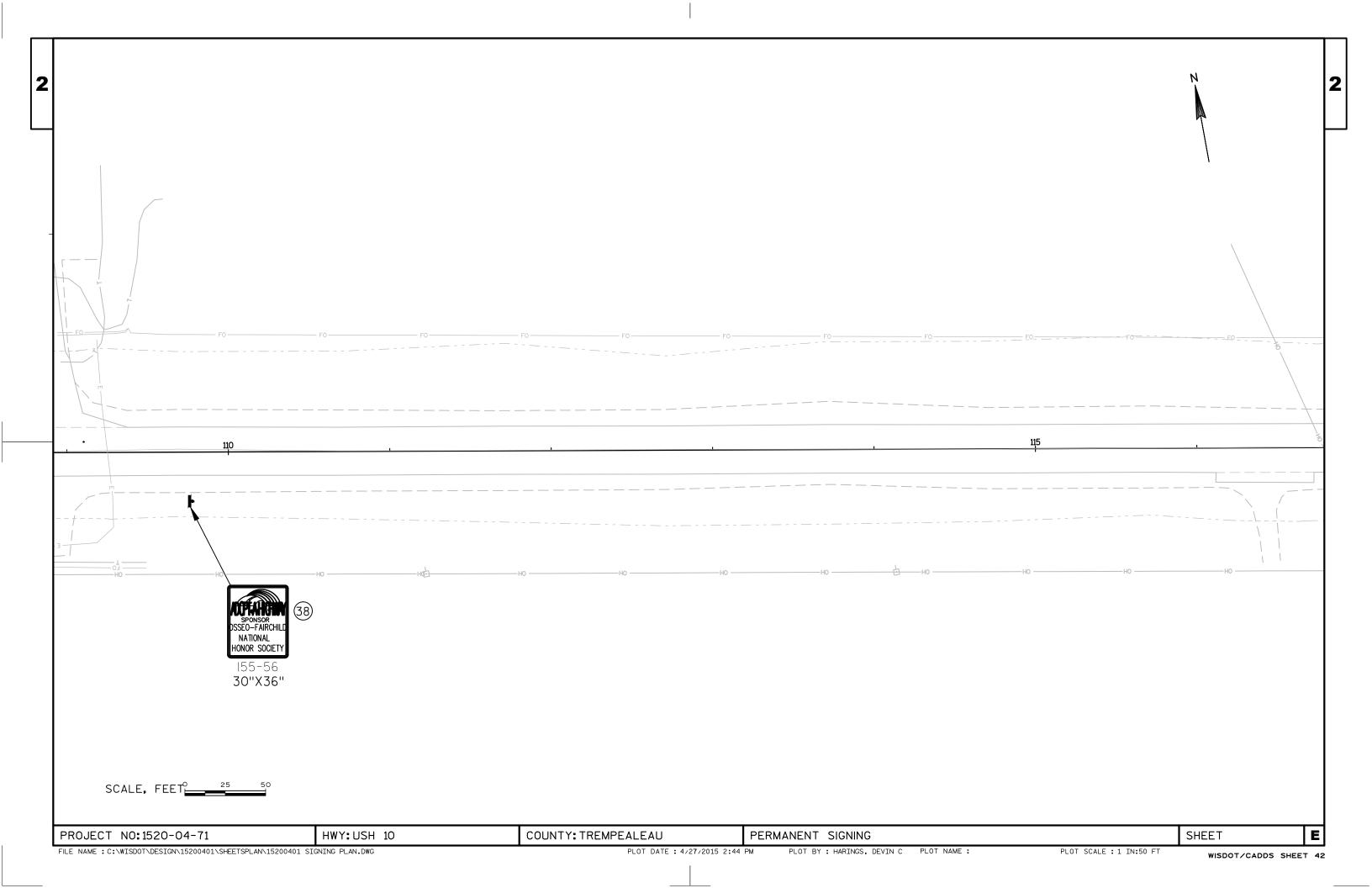


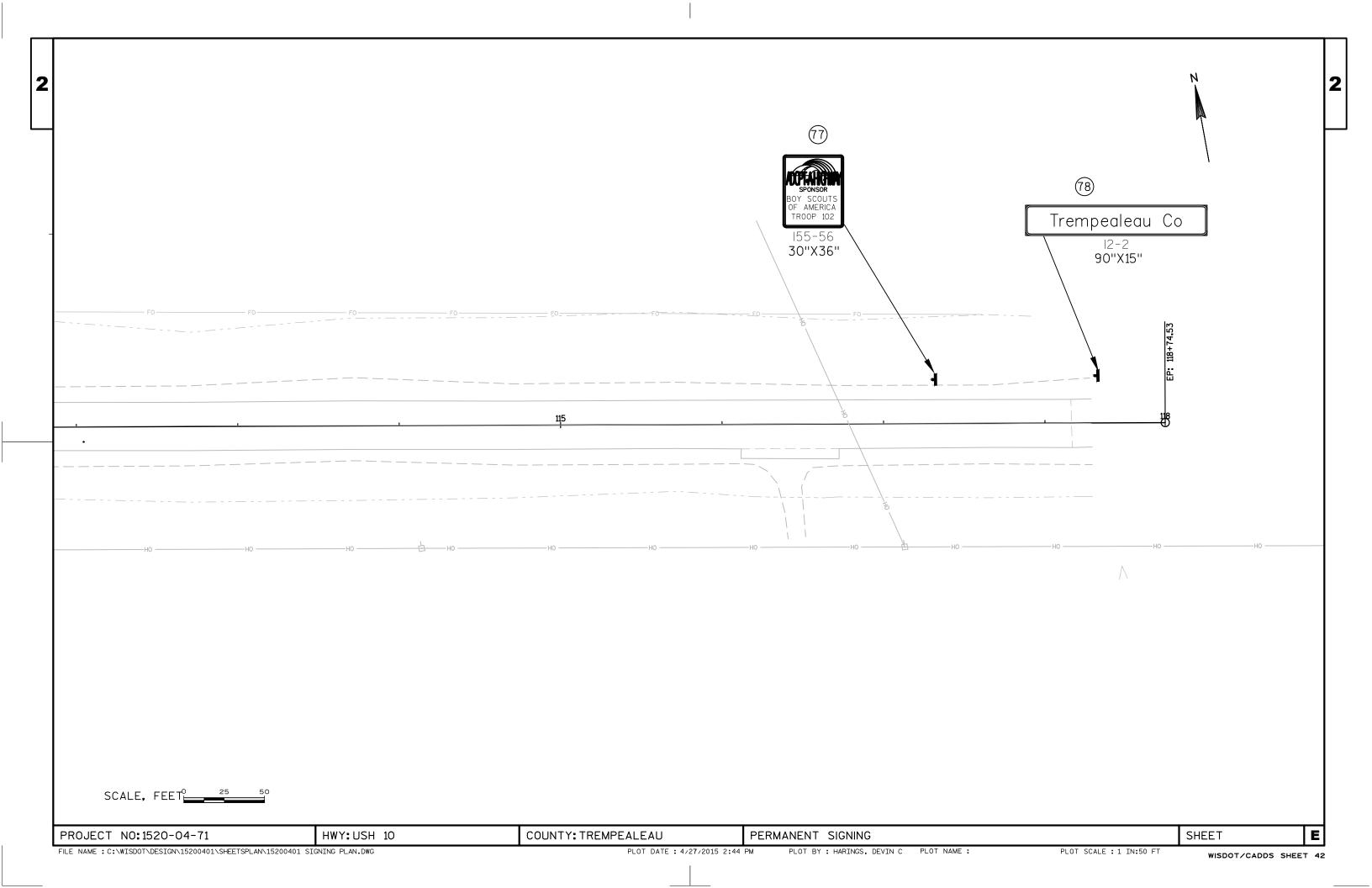


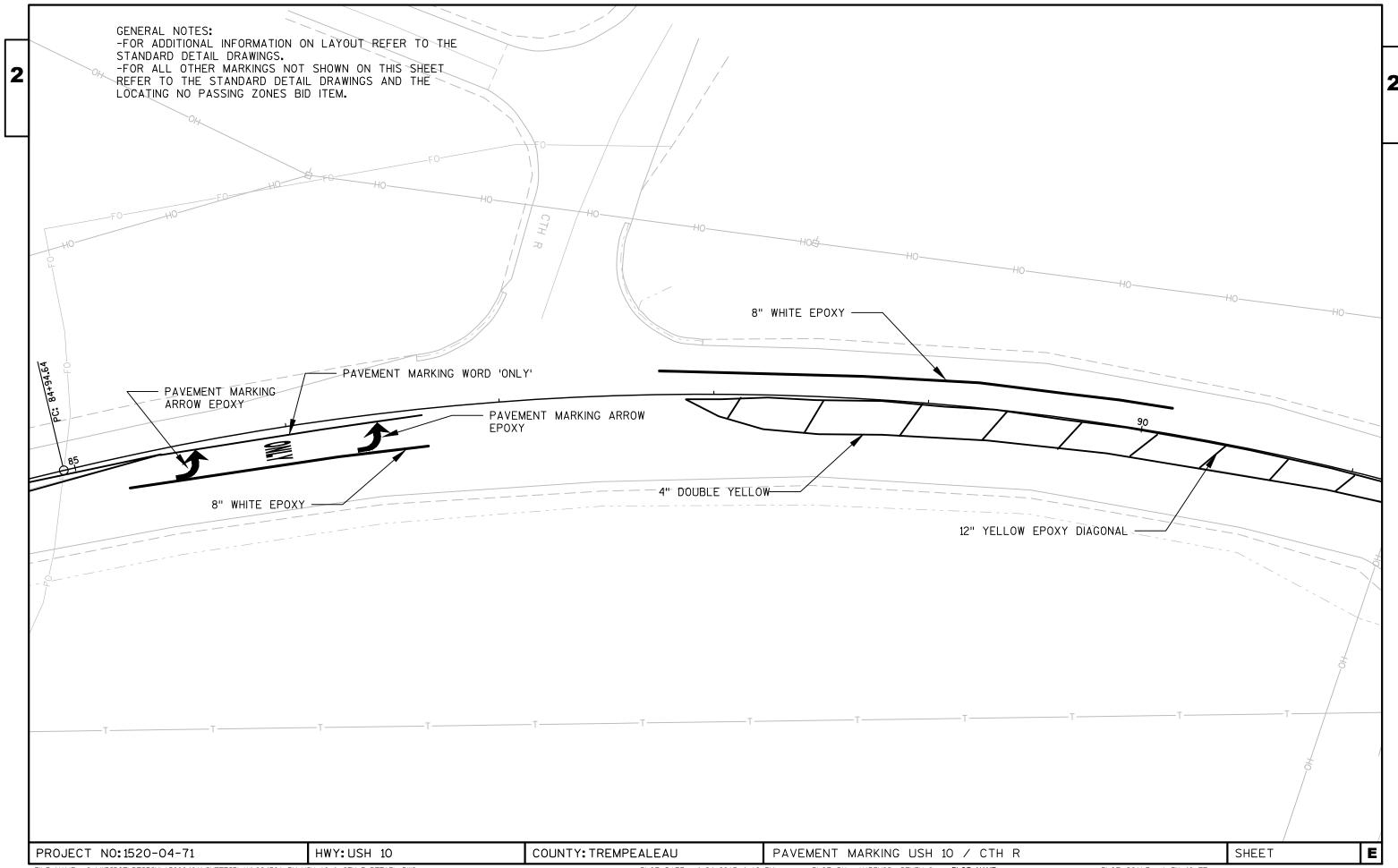


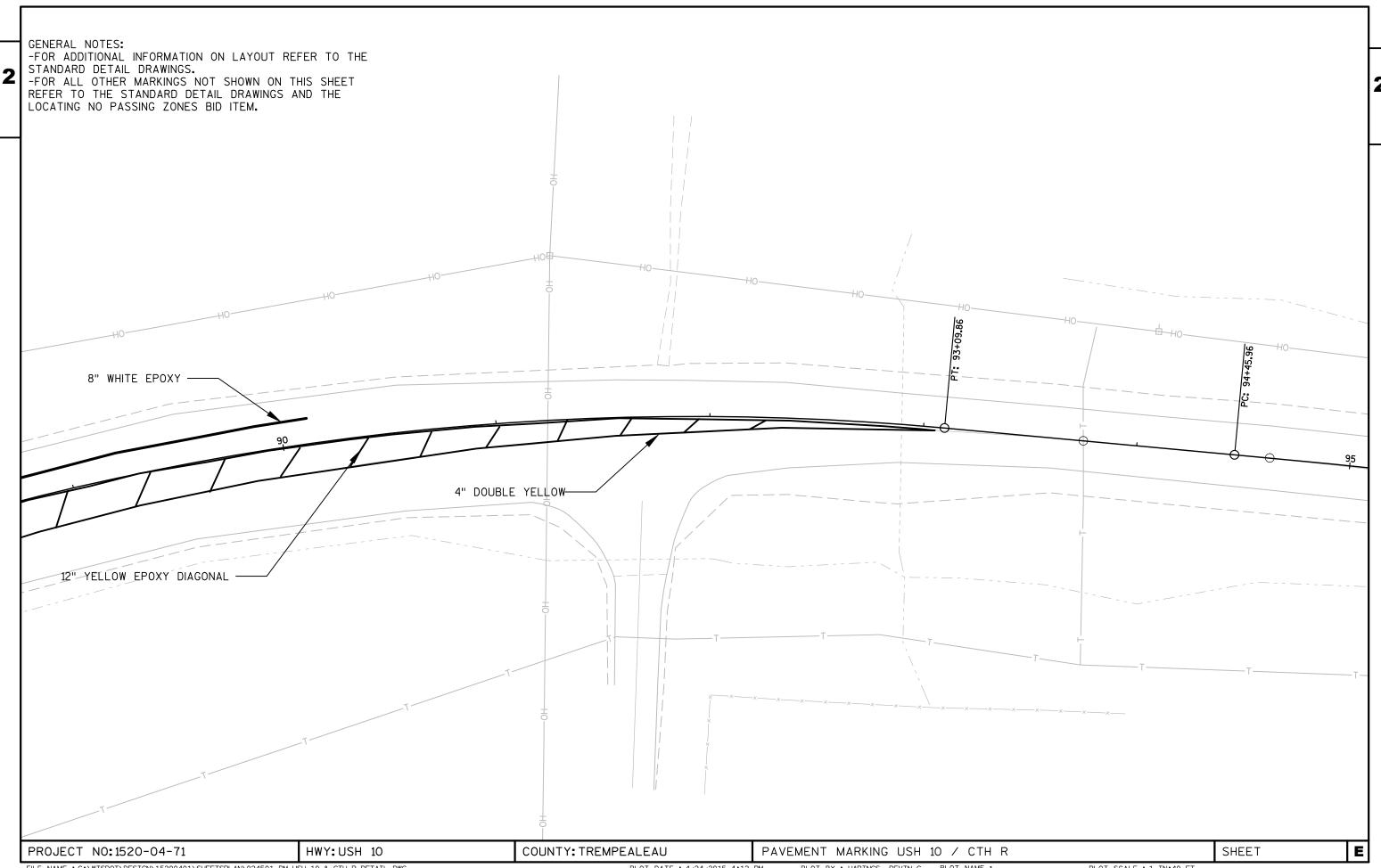


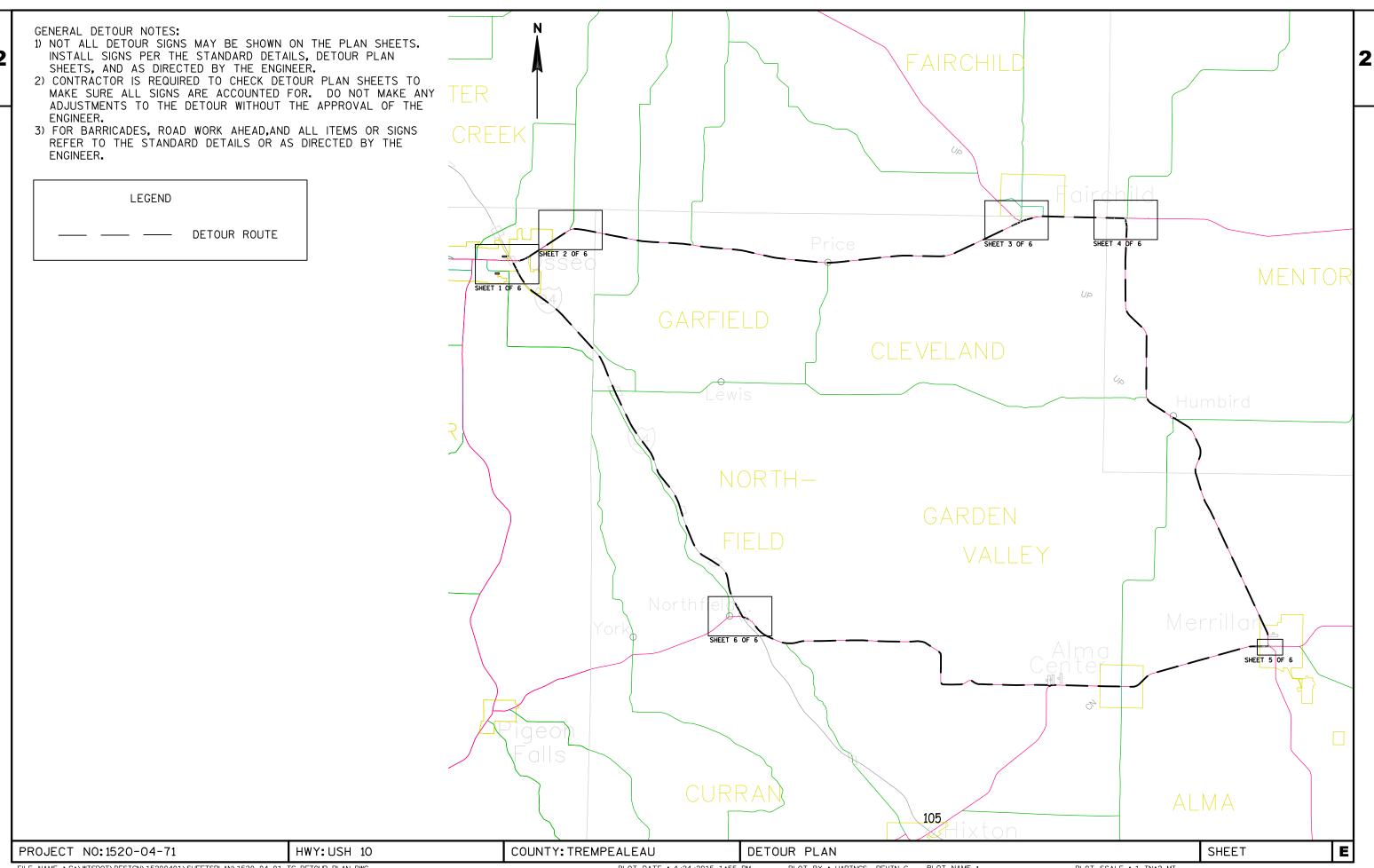


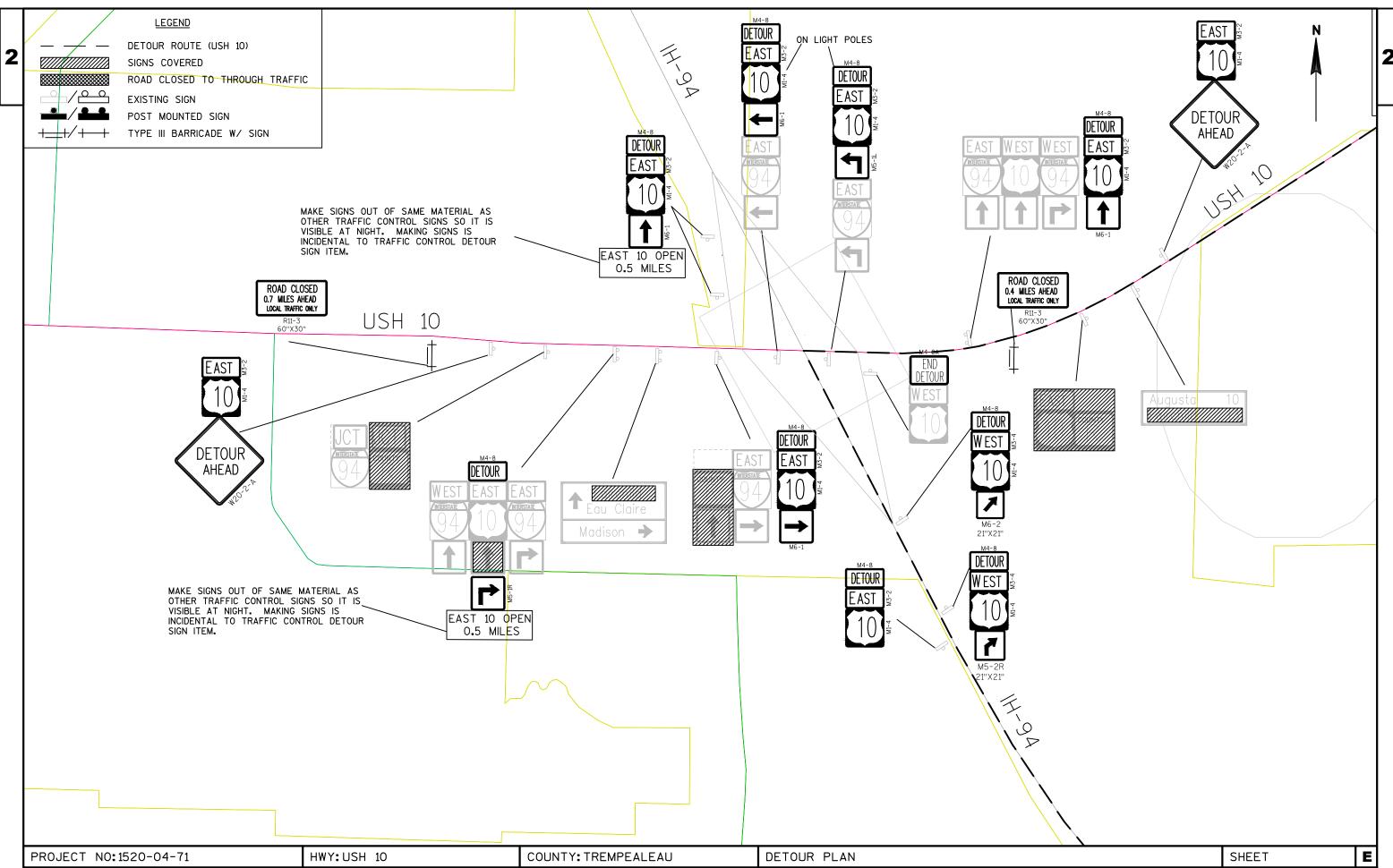


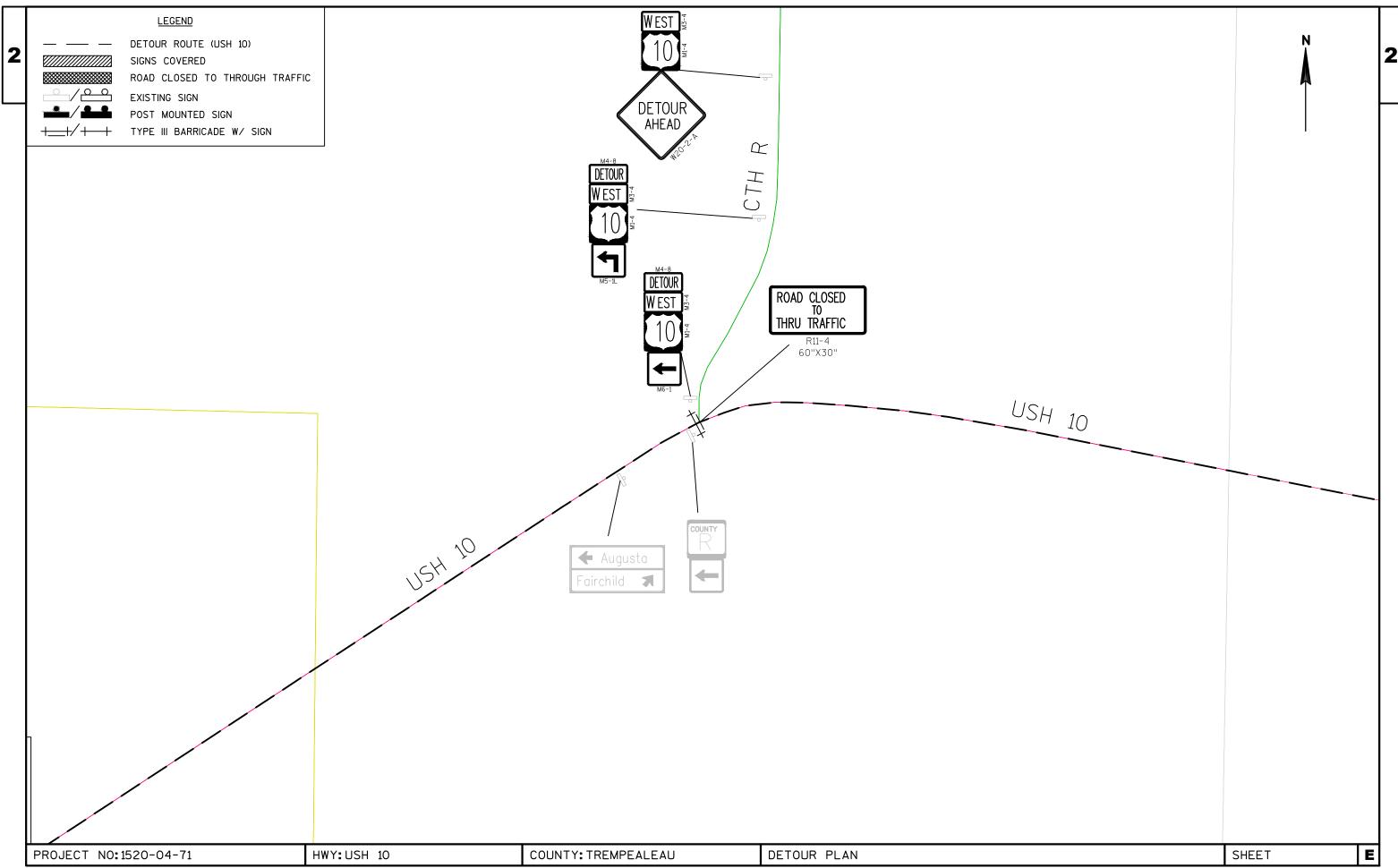


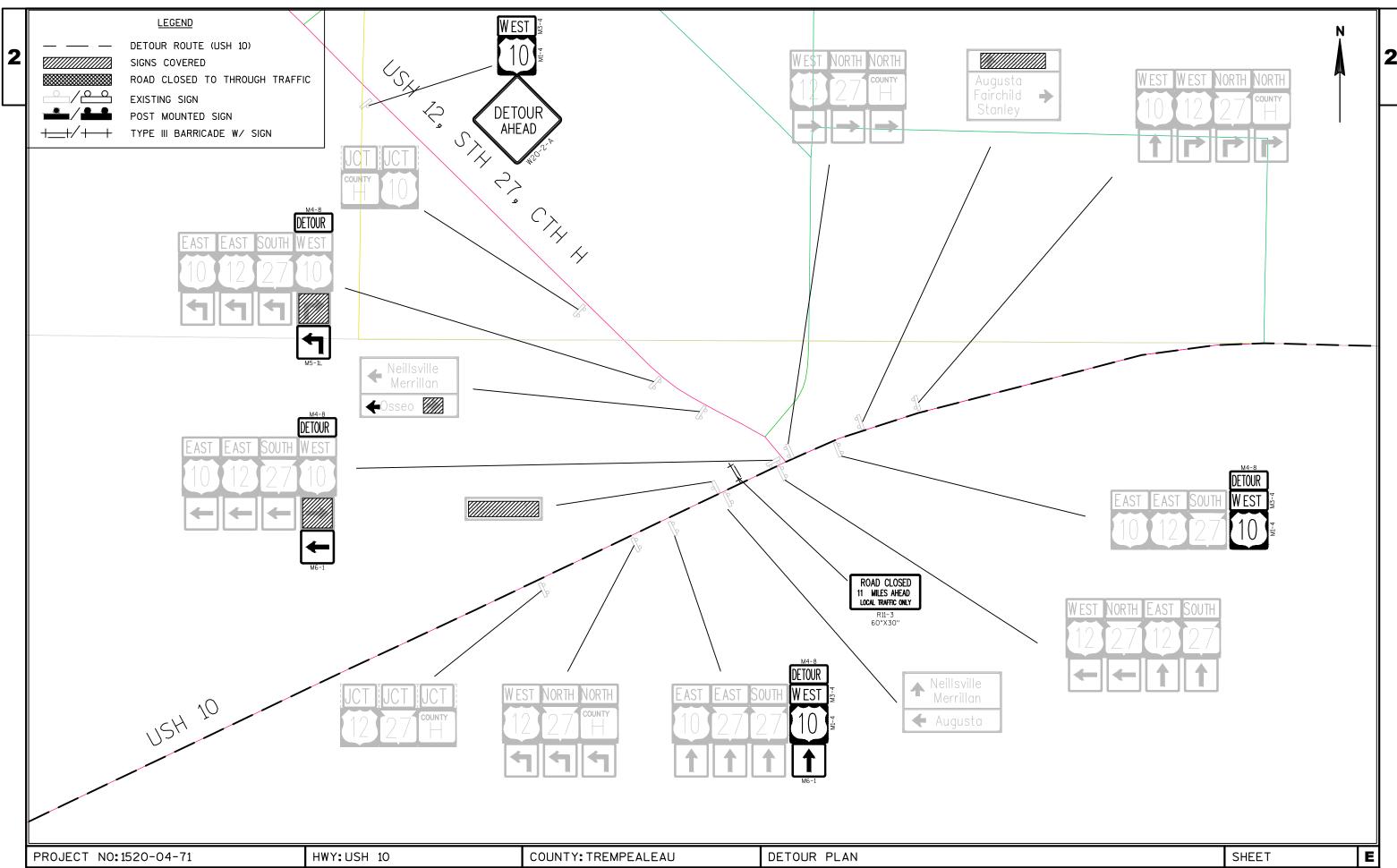


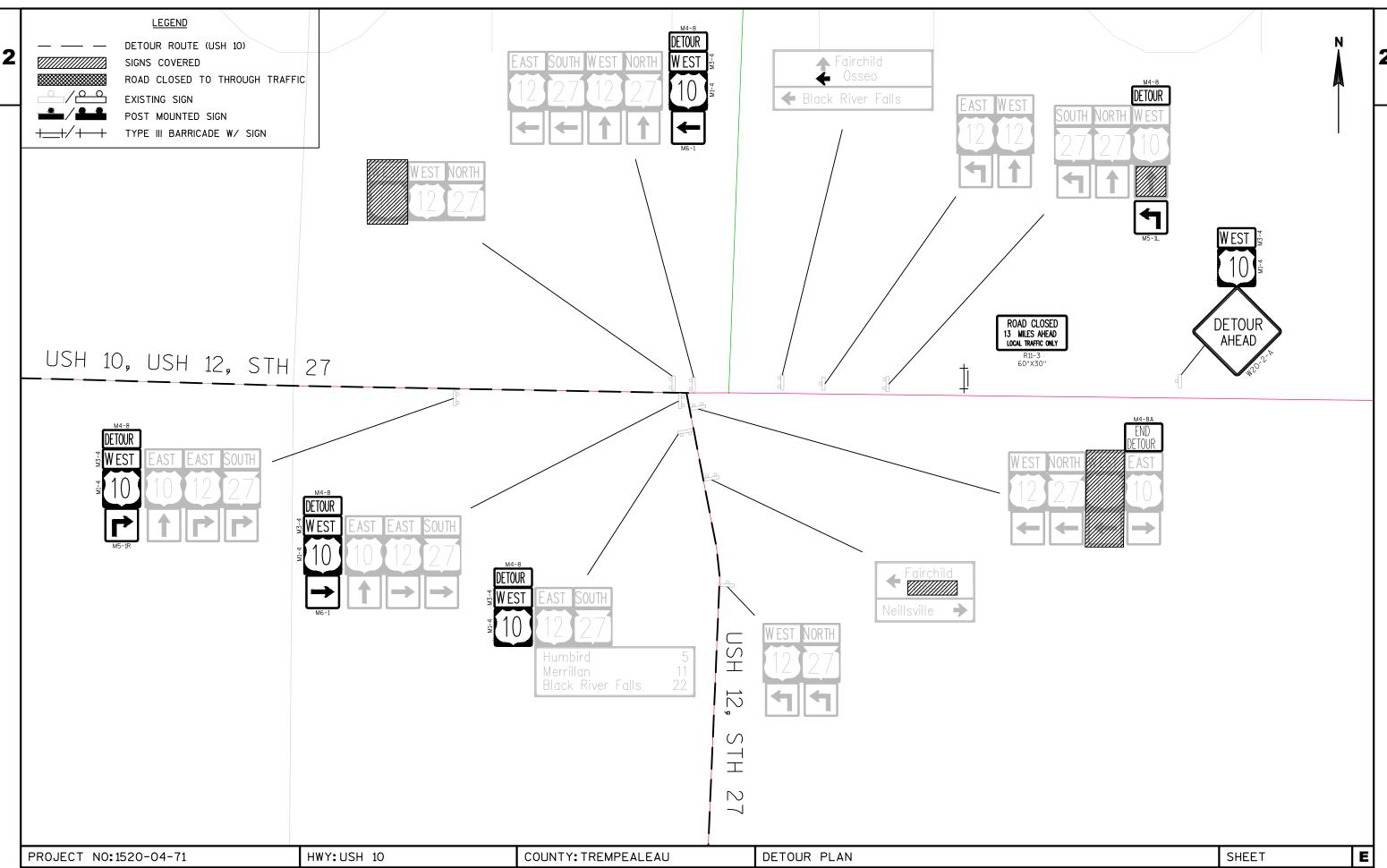


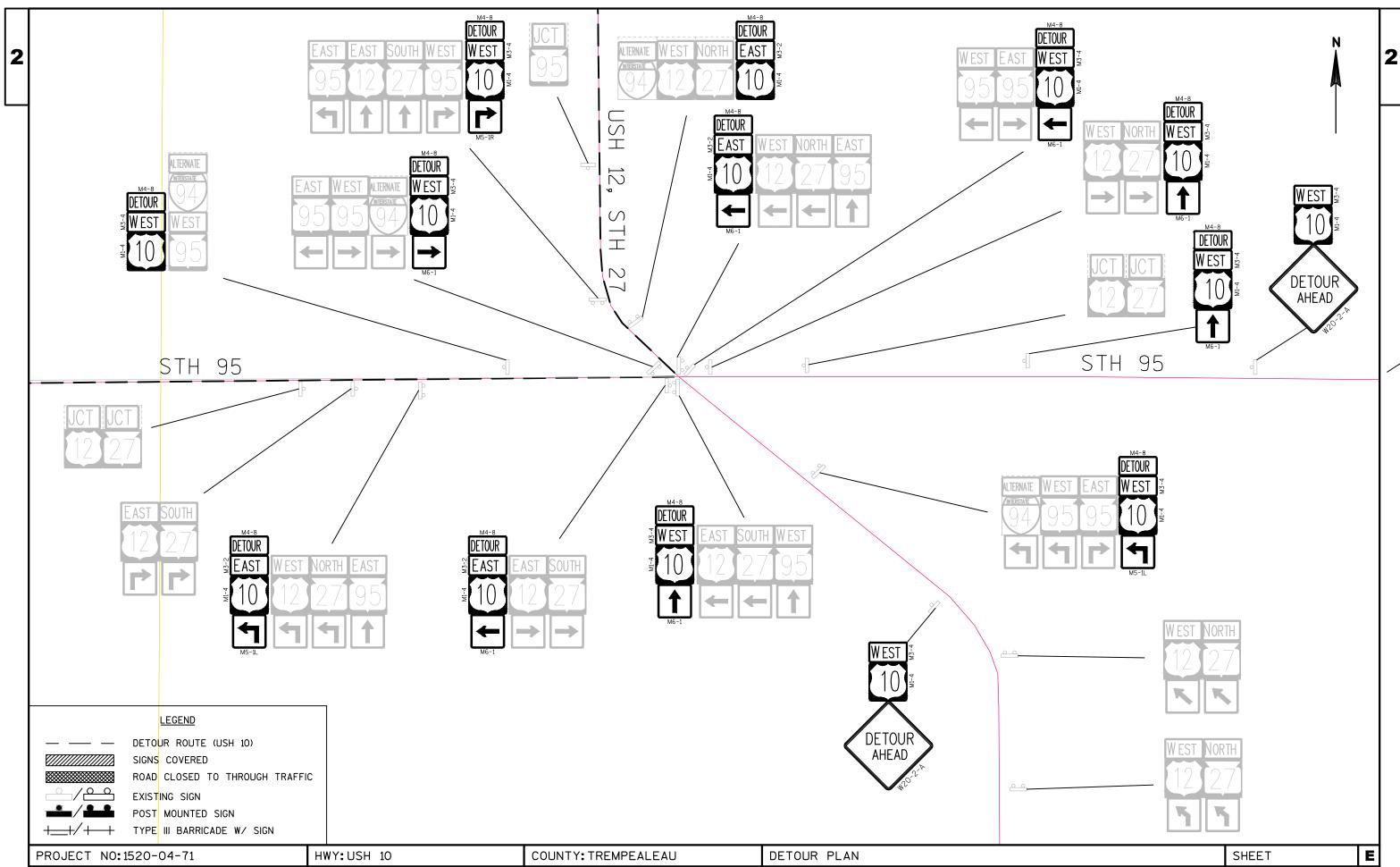


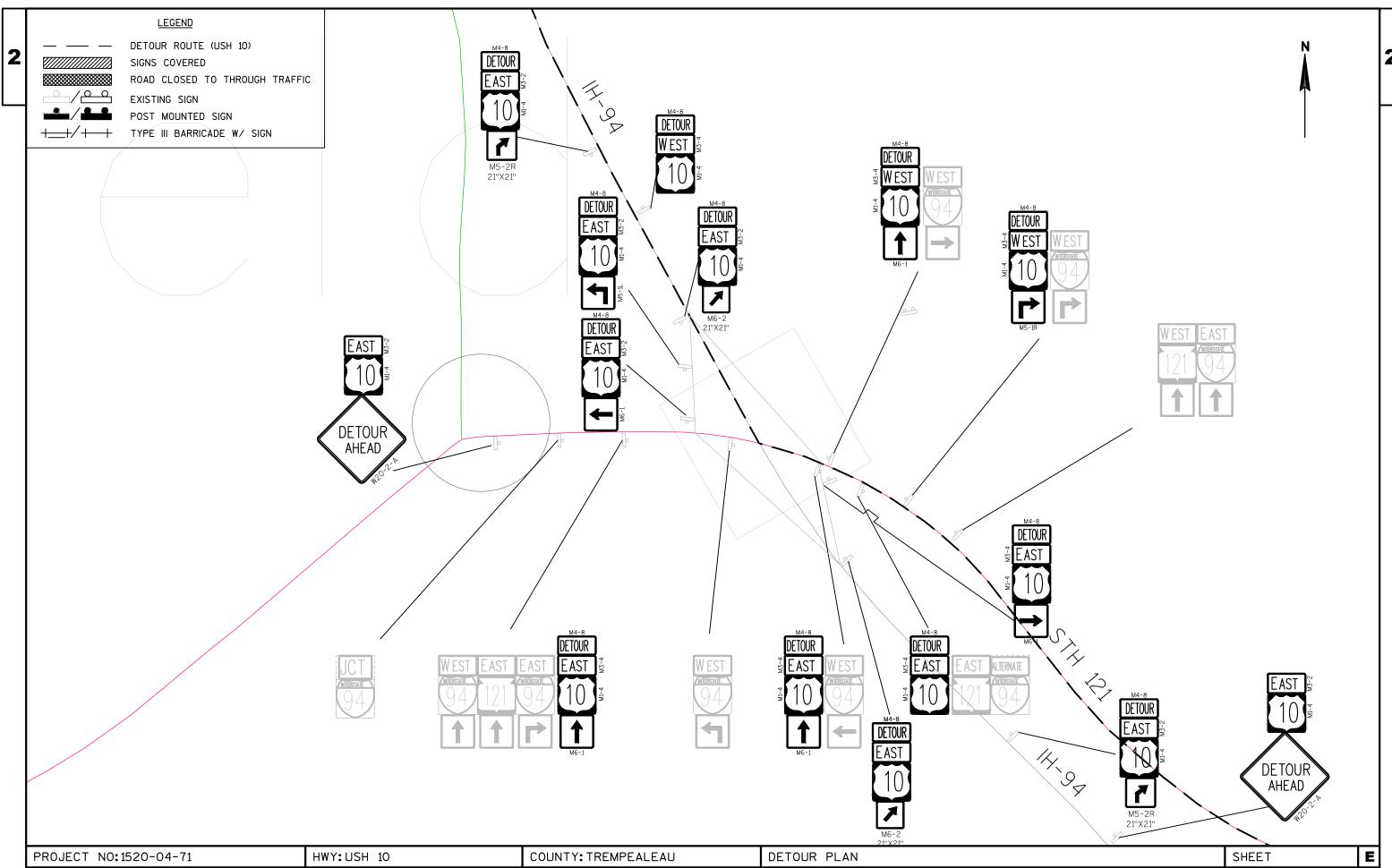












USH 10 EB ALIGNMENT

Tangent	N	516960.653	E	885256.6	Z	Station	05+00
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Arc							
	PC N	516956.828	Е	885586.9		Station	08+30.27
	CC N	519581.652	Е	885617.3			
	PIN	516947.243	Е	886414.4		Station	16+57.86
		TAN	827.585'				
		DB	S 89° 20' 11.01 E'''				
		DA	N 55° 39' 59.58 E'"				
		LChord Distance	1578.577'	Bearing	N 73° 9' 54.29 E"		
		External Distance	127.367'				
		Middle Ordinate	121.473'				
		Radius	2625.000'				
		DEG	2° 10' 57.71'"				
		DELTA	34° 59' 49.42'''				
		LENGTH	1603.387'				
	PT N	517414.008	Е	887097.8		Station	24+33.66
Tangent	N	517414.008	Е	887097.8	Z	Station	24+33.66
	Distance	499.274'	Bearing	N 55° 39' 5	59.58 E"		
Arc							
	PC N	517695.602	Е	887510.1		Station	29+32.93
	CC N	504149.685	E	896762.1			
	PIN	517793.809	E	887653.9		Station	31+07.05
		TAN	174.124'				
		DB	N 55° 39' 59.58 E'"				
		DA	N 56° 52' 58.29 E'"				
		LChord Distance	348.227'	Bearing	N 56° 16' 28.94 E"		
		External Distance	.924'				
		Middle Ordinate	.924'				
		Radius	16404.000'				
		DEG	0° 20' 57.41'''				
		DELTA	1° 12' 58.71'''				
		LENGTH	348.234'				
	PT N	517888.942	Е	887799.7		Station	32+81.16

USH 10 EB ALIGNMENT CON'T

Tangent	N	517888.942	E	887799.7	Z	Station	32+81.16
	Distance	168.974'	Bearing	N 56° 52' 5	58.29 E"		
Arc							
Arc	PC N	517981.262	С	887941.2		Station	34+50.14
	CC N	531720.519		878978.9		Station	34+30.14
	PIN	518100.228		888123.6		Station	36+67.89
	PIN	TAN	217.747'	000123.0		Station	30+07.63
		DB	N 56° 52' 58.29 E'''				
		DA	N 55° 21' 42.69 E'''				
		LChord Distance	435.456'	Bearing	N 56° 7' 20.49 E"		
		External Distance		Dearing	N 30 7 20.43 L		
		Middle Ordinate					
		Radius	16404.000'				
		DEG	0° 20' 57.41'''				
			1° 31' 15.61'''				
		DELTA					
	DT N	LENGTH 510222 004	435.469'	000202.0		C+-+:	20.05.61
	PT N	518223.994	L	888302.8		Station	38+85.61
Tangent	N	518223.994	E	888302.8	Z	Station	38+85.61
	Distance	67.341'	Bearing	N 55° 21' 4	2.69 E"		
Tangent	N	518262.27	F	888358.2	7	Station	39+52.95
		4541.696'	Bearing	N 55° 38' 1		J. C.	00 102.00
Arc							
	PC N	520825.732		892107.3		Station	84+94.64
	CC N	519774.068		892826.3			
	PIN	521063.984		892455.7		Station	89+16.76
		TAN	422.112'				
		DB	N 55° 38' 14.63 E'''				
		DA	S 87° 41' 58.62 E'''				
		LChord Distance	801.382'	Bearing	N 73° 58' 8.01 E"		
		External Distance	68.108'				
		Middle Ordinate	64.652'				
		Radius	1274.000'				
		DEG	4° 29' 50.33'''				
		DELTA	36° 39' 46.76'"				
		LENGTH	815.220'				
	PT N	521047.041	E	892877.5		Station	93+09.86

PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU ALIGNMENT DETAILS SHEET: **E**

2

USH 10 EB ALIGNMENT CON'T

Tangent	N	521047.041	E	892877.5	Z	Station	93+09.86
	Distance	136.091'	Bearing	S 87° 41' 58	8.62 E"		
Arc							
	PC N	521041.579	E	893013.5		Station	94+45.96
	CC N	513408.735	Е	892706.8			
	PIN	521020.56	E	893536.7		Station	99+69.6
		TAN	523.644'				
		DB	S 87° 41' 58.62 E'''				
		DA	S 79° 51' 24.39 E'''				
		LChord Distance	1044.836'	Bearing	S 83° 46' 41.5 E"		
		External Distance	17.927'				
		Middle Ordinate	17.885'				
		Radius	7639.000'				
		DEG	0° 45' 0.15'"				
		DELTA	7° 50' 34.23'"				
		LENGTH	1045.652'				
	PT N	520928.342	Е	894052.1		Station	104+91.61
Tangent	N	520928.342	F	894052.1	7	Station	104+91.61
rangent	Distance	1382.923'	Bearing	S 79° 51' 2		Station	104131.01
End	N	520684.796	E	895413.5	Z	Station	118+74.53

PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU ALIGNMENT DETAILS SHEET: **E**

USH 10 WB ALIGNMENT

N	516990.114	L	885241	L	Station	04+84
Distance	357.248'		S 89° 17' 3			
		J				
PC N	516985.711	E	885598.2		Station	08+41.25
CC N	519593.513	E	885630.4			
PIN	516975.592	E	886419.2		Station	16+62.25
	TAN	821.005'				
	DB	S 89° 17' 3	7.66 E'''			
	DA	N 55° 45' 2	28.53 E'"			
	LChord Distance	1566.236'	Bearing	N 73° 13' 55.44 E"		
	External Distance					
	Middle Ordinate	120.353'				
	Radius	2608.000'				
	DEG	2° 11' 48.9	3'"			
	DELTA	34° 56′ 53.	82'"			
	LENGTH	1590.783'				
PT N	517437.564	Е	887097.9		Station	24+32.03
N	517437.564	E	887097.9	Z	Station	24+32.03
Distance	362.592'	Bearing	N 55° 45' 2	28.53 E"		
PC N	517641.591	F	887397.6		Station	27+94.62
					Station	27131102
					Station	30+96.08
1111			00701010		Station	30130100
			28.53 F'''			
				N 56° 48' 39.61 F"		
			2 20	33 13 33.02 2		
			1			
	LENGTH	602.854'				
PT N			887902.1		Station	33+97.48
N	517071 E7 <i>G</i>	Е	QQ7002 1	7	Station	22±07 40
					Station	33+97.48
	PC N CC N PI N PT N N Distance PC N CC N PI N	PC N 516985.711 CC N 519593.513 PI N 516975.592	PC N 516985.711 E CC N 519593.513 E PI N 516975.592 E TAN 821.005' DB \$89° 17' 3 DA N 55° 45' 2 LChord Distance 1566.236' External Distance 126.175' Middle Ordinate 120.353' Radius 2608.000' DEG 2° 11' 48.9 DELTA 34° 56' 53. LENGTH 1590.783' PT N 517437.564 E N 517437.564 E N 517437.564 E Distance 362.592' Bearing PC N 517641.591 E CC N 504084.242 E PI N 517811.22 E TAN 301.461' DB N 55° 45' 2 EXTERNAL 301.461' DB N 55° 45' 2 EXTERNAL 301.461' DB N 57° 51' 5 DA N 57° 51' 5 DELTA 2° 6' 22.16 LENGTH 602.854' PT N 517971.576 E	PC N 516985.711 E 885598.2 CC N 519593.513 E 885630.4 PI N 516975.592 E 886419.2 TAN 821.005' DB S 89° 17' 37.66 E''' DA N 55° 45' 28.53 E''' LChord Distance 1566.236' Bearing 120.353' Radius 2608.000' DEG 2° 11' 48.93''' DELTA 34° 56' 53.82''' LENGTH 1590.783' PT N 517437.564 E 887097.9 N 517437.564 E 887097.9 N 517437.564 E 887097.9 PC N 517641.591 E 887397.6 CC N 504084.242 E 896625.8 PI N 517811.22 E 887646.8 TAN 301.461' DB N 55° 45' 28.53 E''' LChord Distance 602.820' Bearing External Distance 2.770' Middle Ordinate Radius 16400.000' DEG 0° 20' 57.71''' DELTA 2° 6' 22.16''' DELTA 2° 6' 22.16''' ENSTPON 517971.576 E 887902.1	PC N 516985.711 E 885598.2 CC N 519593.513 E 885630.4 PI N 516975.592 E 886419.2 TAN 821.005' DB \$89°17'37.66 E"' DA N 55°45'28.53 E"' LChord Distance 126.775' Middle Ordinate 120.353' Radius 2608.00' DEG 2°11'48.93''' DELTA 34°56'53.82''' LENGTH 1590.783' PT N 517437.564 E 887097.9 N 517437.564 E 887097.9 N 517437.564 E 887097.9 PC N 517641.591 E 887397.6 CC N 504084.242 E 896625.8 PI N 517811.22 E 887646.8 TAN 301.461' DB N 55° 45'28.53 E''' LChord Distance 602.820' Bearing N 56° 48' 39.61 E'' External Distance 2.770' Middle Ordinate 2.770' Middle Ordinate 2.770' Middle Ordinate 2.770' Radius 16400.000' DEG 0° 20'57.71''' DELTA 2° 6'22.16''' LENGTH 602.854' PT N 517971.576 E 887902.1	PC N 516985.711 E 885598.2 Station CC N 519593.513 E 885630.4 PI N 516975.592 E 886419.2 Station TAN 821.005' DB S 89' 17' 37.66 E''' DA N 55° 45' 28.53 E''' LChord Distance 1566.236' Bearing N 73° 13' 55.44 E'' External Distance 120.353' Radius 2608.000' DEG 2° 11' 48.93''' DELTA 34' 56' 53.82''' LENGTH 1590.783' PT N 517437.564 E 887097.9 Station N 517437.564 E 887097.9 Z Station N 517641.591 E 887097.9 Z Station N 55° 45' 28.53 E''' DDA N 55° 45' 28.53 E''' DA N 57° 51' 50.69 E''' LChord Distance 602.820' Bearing N 56° 48' 39.61 E'' External Distance 2.770' Middle Ordinate 2.770' Middle Ordinate 2.770' Radius 16400.000' DEG 0° 20' 57.71''' DELTA 2° 6' 22.16'''' LENGTH 602.854' PT N 517971.576 E 887902.1 S Station

USH 10 WB ALIGNMENT CON'T

Arc								
	PC N	518036.369	Е	888005.3			Station	35+19.28
	CC N	531923.702	E	879281.6				
	PIN	518151.962	E	888189.3			Station	37+36.59
		TAN	217.309'					
		DB	N 57° 51' 5	60.69 E'"				
		DA	N 56° 20' 4	14.77 E'"				
		LChord Distance	434.579'	Bearing	N 57° 6' 17	'.73 E"		
		External Distance	1.440'					
		Middle Ordinate	1.440'					
		Radius	16400.000	ı				
		DEG	0° 20' 57.7	1'''				
		DELTA	1° 31' 5.92	III				
		LENGTH	434.592'					
	PT N	518272.39	E	888370.2			Station	39+53.88
Tangent	N	518272.39	Е	888370.2	Z		Station	39+53.88
	Distance	128.888'	Bearing	N 56° 20' 4	14.77 E"			
End	N	518343.817	E	888477.4	Z		Station	40+82.76

PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU ALIGNMENT DETAILS SHEET: **E**

DATE 03	AUG15	EST	IMAT	E OF QUAN		
LINE	LTEM	LTEM DESCRIPTION	IINI T	TOTAL	1520-04-71	
NUMBER	I TEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTI TY	
0010	203. 0100	Removing Small Pipe Culverts	EACH	3.000	3. 000	
0020	204. 0110	Removing Asphaltic Surface	SY	255. 000	255. 000	
0030	204. 0115	Removing Asphaltic Surface Butt Joints	SY	139. 000	139. 000	
0040	204. 0125	Removing Asphaltic Surface Milling	TON	8, 961. 000	8, 961. 000	
0050	204. 0165	Removing Guardrail	LF	800.000	800.000	
0060	204. 0185	Removing Masonry	CY	1. 000	1. 000	
0070	204. 0245	Removing Storm Sewer (size) 01.15-INCH	LF	60.000	60.000	
0800	213. 0100	Finishing Roadway (project) 01.	EACH	1.000	1. 000	
		1520-04-71				
0090	305. 0110	Base Aggregate Dense 3/4-Inch	TON	1, 190. 000	1, 190. 000	
0100	305. 0120	Base Aggregate Dense 1 1/4-Inch	TON	285.000	285. 000	
0110	305.0500	Shapi ng Shoul ders	STA	162. 000	162. 000	
0120	416. 0610	Drilled Tie Bars	EACH	114. 000	114. 000	
0130	416. 0620	Drilled Dowel Bars	EACH	107.000	107. 000	
0140		S Incentive IRI Ride	DOL	6, 080. 000	6, 080. 000	
0150	455. 0605	Tack Coat	GAL	4, 975. 000	4, 975. 000	
0160	460. 2000	Incentive Density HMA Pavement	DOL	6, 590. 000	6, 590. 000	
0170	465. 0105	Asphaltic Surface	TON	500.000	500. 000	
0180	465. 0110	Asphaltic Surface Patching	TON	50. 000	50. 000	
0190	465. 0120	Asphaltic Surface Driveways and Field	TON	25. 000	25. 000	
0170	100.0120	Entrances	1014	23.000	23.000	
0200	522. 0130	Culvert Pipe Reinforced Concrete Class	LF	120. 000	120. 000	
	, 0.00	III 30-Inch		0, 000	3. 333	
0010	F00 0440			240, 222	240.000	
0210	522. 0142	Culvert Pipe Reinforced Concrete Class	LF	340.000	340. 000	
0220	E22 101E	III 42-Inch	EACH	1. 000	1. 000	
0220	522. 1015	Apron Endwalls for Culvert Pipe Reinforced Concrete 15-Inch	LACII	1.000	1.000	
0230	522. 1030	Apron Endwalls for Culvert Pipe	EACH	2. 000	2. 000	
0230	322. 1030	Reinforced Concrete 30-Inch	LACIT	2.000	2.000	
0240	522. 1042	Apron Endwalls for Culvert Pipe	EACH	4. 000	4. 000	
02 10	022. 1012	Reinforced Concrete 42-Inch	271011	1. 000	1. 000	
0250	606. 0200	Riprap Medium	CY	50.000	50.000	
0260	608. 0415	Storm Sewer Pipe Reinforced Concrete	LF	60.000	60. 000	
0270	414 0010	Class IV 15-Inch	EACH	10 000	12 000	
0270	614. 0010	Barrier System Grading Shaping Finishing	EACH	12.000	12.000	
0280	614. 0305	Steel Plate Beam Guard Class A	LF	1, 410. 200	1, 410. 200	
0290	614. 0370	Steel Plate Beam Guard Energy Absorbing	EACH	12. 000	12. 000	
0200	614 0400	Terminal		1 250 000	1 250 200	
0300	614. 0400	Adjusting Steel Plate Beam Guard	LF	1, 250. 000	1, 250. 000	
0310	614. 0950	Replacing Guardrail Posts and Blocks	EACH	100.000	100.000	
0320	618. 0100	Maintenance And Repair of Haul Roads	EACH	1. 000	1. 000	
5525	313.0100	(project) 01.1520-04-71		1.000	1. 000	
0330	619. 1000	Mobilization	EACH	1. 000	1. 000	
0340	625. 0500	Sal vaged Topsoi I	SY	596. 000	596. 000	
0350	627. 0200	Mul chi ng	SY	226. 000	226. 000	
0360	628. 1104	Erosi on Bal es	EACH	80.000	80. 000	
0370	628. 1504	Silt Fence	LF	1, 920. 000	1, 920. 000	
0380	628. 1520	Silt Fence Maintenance	LF	100.000	100.000	
0390	628. 1905	Mobilizations Erosion Control	EACH	3. 000	3. 000	
0400	628. 1910	Mobilizations Emergency Erosion Control	EACH	2. 000	2. 000	
0410	628. 2004	Erosion Mat Class I Type B	SY	640.000	640. 000	
	628. 7504	Temporary Ditch Checks	LF	190. 000	190. 000	
0420		Cultivant Dina Charles	EACH	35. 000	35.000	
0420 0430	628. 7555	Cul vert Pi pe Checks				
0420		Fertilizer Type B Seeding Mixture No. 20	CWT LB	1. 000 16. 600	1. 000 16. 600	

DATE 03 LINE	BAUG15	EST	IMATE	E O F Q U A N	T I T I E S 1520-04-71	
NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	QUANTI TY	
0460	633. 5200	Markers Culvert End	EACH	9. 000	9. 000	
0470 0480	634. 0616	Posts Wood 4x6-Inch X 16-FT Signs Type II Reflective H	EACH SF	53. 000 478. 880	53. 000 478. 880	
0480	637. 2210 637. 2215	Signs Type II Reflective H Folding	SF SF	37. 300	37. 300	
0500	637. 2230	Signs Type II Reflective F	SF	131. 750	131. 750	
0510	638. 2602	Removing Signs Type II	EACH	80.000	80.000	
0520 0530	638. 3000 642. 5001	Removing Small Sign Supports Field Office Type B	EACH EACH	55. 000 1. 000	55. 000 1. 000	
0540	643. 0100	Traffic Control (project) 01. 1520-04-71	EACH	1. 000	1. 000	
0550	643. 0300	Traffic Control Drums	DAY	1, 500. 000	1, 500. 000	
0560	643. 0420	Traffic Control Barricades Type III	DAY	476. 000	476. 000	
0570	643. 0705	Traffic Control Warning Lights Type A	DAY	560.000	560. 000	
0580	643. 0715	Traffic Control Warning Lights Type C	DAY	196.000	196. 000	
0590 0600	643. 0800 643. 0900	Traffic Control Arrow Boards Traffic Control Signs	DAY DAY	56. 000 252. 000	56. 000 252. 000	
	043.0700	-	DAI	232.000	232.000	
0610	643. 0920	Traffic Control Covering Signs Type II	EACH	15. 000	15. 000	
0620	643. 2000	Traffic Control Detour (project) 01.	EACH	1. 000	1. 000	
0630	643. 3000	1520-04-71 Traffic Control Detour Signs	DAY	2, 982. 000	2, 982. 000	
0640	645. 0120	Geotextile Fabric Type HR	SY	174. 000	174. 000	
0650	646. 0106	Pavement Marking Epoxy 4-Inch	LF	15, 826. 000	15, 826. 000	
				· 		
0660	646. 0126	Pavement Marking Epoxy 8-Inch	LF	625.000	625. 000	
0670 0680	646. 0406 646. 0600	Pavement Marking Same Day Epoxy 4-Inch Removing Pavement Markings	LF LF	9, 569. 000 175. 000	9, 569. 000 175. 000	
0680	646. 0600 647. 0166	Pavement Marking Arrows Epoxy Type 2	EACH	2. 000	2. 000	
0700	647. 0356	Pavement Marking Words Epoxy	EACH	1. 000	1. 000	
0740				/ 40, 000		
0710 0720	647. 0726 648. 0100	Pavement Marking Diagonal Epoxy 12-Inch Locating No-Passing Zones	LF MI	640. 000 1. 520	640. 000 1. 520	
0720	649. 0100	Temporary Pavement Marking 4-Inch	LF	19, 138. 000	19, 138. 000	
0740	649. 0400	Temporary Pavement Marking Removable	LF	1, 000. 000	1, 000. 000	
		Tape 4-Inch				
0750	650. 4000	Construction Staking Storm Sewer	EACH	2. 000	2. 000	
0760	650. 6000	Construction Staking Pipe Culverts	EACH	3. 000	3. 000	
0770	650. 8000	Construction Staking Resurfacing	LF	14, 918. 000	14, 918. 000	
		Reference				
0780	650. 9910	Construction Staking Supplemental	LS	1. 000	1. 000	
0790	690. 0150	Control (project) 01.1520-04-71 Sawing Asphalt	LF	661. 000	661. 000	
0800	690. 0150	Sawing Asphart Sawing Concrete	LF	580. 000	580. 000	
0810	ASP. 1T0A	On-the-Job Training Apprentice at \$5.	HRS	1, 200. 000	1, 200. 000	
0820	ASP. 1TOG	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000	
0830	SPV. 0105	Special 01. Preparation of Foundation	LS	1. 000	1.000	
00.40	CDV 010F	for Asphaltic Paving Special	1.0	1 000	1 000	
0840	SPV. 0105	Special 02. Grading, Shaping and Finishing Driveway Removal 34+60	LS	1. 000	1. 000	
0850	SPV. 0105	Special 03. Grading, Shaping and	LS	1. 000	1. 000	
		Finishing Driveway Removal 47+30 LT				
0860	SPV. 0105	Special 04. Grading, Shaping and	LS	1. 000	1. 000	
5500	J. V. 010J	Finishing Driveway Removal 47+30 RT		1.000	1.000	
0870	SPV. 0105	Special 05. Grading, Shaping and	LS	1. 000	1. 000	
0000	CDV 040E	Finishing Relocating Driveway 71+50 LT	1.0	4 000	4 000	
0880	SPV. 0105	Special 06. Grading, Shaping and Finishing Relocating Driveway 71+50 RT	LS	1. 000	1. 000	
0890	SPV. 0180	Special 01. Concrete Pavement Repair	SY	138. 000	138. 000	
- · ·		Dowel ed				

DATE 0: LINE NUMBER 0900	I TEM SPV. 0180	ITEM DESCRIPTION Special O2. 36" Concrete Centerline Repair	ESTIMATI UNIT SY	TOTAL 38.000	T I T I E S 1520-04-71 QUANTITY 38.000
0910	SPV. 0195	Special O1. HMA Pavement Type E-3 Special	TON	10, 293. 000	10, 293. 000

REMOVING SMALL PIPE CULVERTS

			203. 0100
CATEGORY	STATION TO STATION	LOCATI ON	EACH
			_
0010	39+00	USH 10	1
0010	58+90	USH 10	1
0010	73+80	USH 10	1
		TOTAL 0010	3

REMOVING ASPHALTIC SURFACE BUTT JOINTS

			204. 0115	
CATEGORY	STATION TO STATION	LOCATI ON	SY	REMARKS
0010	38+10	USH 10	18	MAI NLI NE
0010	51+60	USH 10, LT	12	RI DGE VI EW RD
0010	51+60	USH 10, RT	12	SI DE RD
0010	87+20	USH 10, LT	17	CTH R
0010	87+97	USH 10	42	MAI NLI NE
0010	91+70	USH 10, RT	13	HOGSTAD RD
0010	108+80	USH 10, LT	15	SI DE RD
0010	118+20	USH 10	10	MAI NLI NE
		TOTAL 0010	0 139	

REMOVING ASPHALTIC SURFACE

			204. 0110	
CATEGORY	STATION TO STATION	LOCATI ON	SY	REMARKS
0010	38+90 - 39+10	USH 10	85	CULVERT REPLACEMENT
0010	58+80 - 59+00	USH 10	85	CULVERT REPLACEMENT
0010	73+70 - 73+90	USH 10	85	CULVERT REPLACEMENT
		TOTAL (0010 255	

PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU MISCELLANEOUS QUANTITIES SHEET: **E**

FILE NAME : N:\PDS\...\030200_mq.pptx PLOT BY : A.R.H. PLOT NAME : PLOT NAME : PLOT SCALE : 1:1

REMOVING ASPHALTIC SURFACE MILLING

CATEGORY	STATION TO STATION	LOCATI ON	204. 0125 TON	REMARKS
0010 0010	38+03 - 87+97 87+97 - 118+20	USH 10 USH 10	7691 1270	MAINLINE, BEAM GUARD, SIDE ROADS MAINLINE, TRANSITION AREA, SIDE ROADS
		TOTAL 0010	8961	

REMOVING GUARDRAIL

				204. 0165	
CATEGORY	STATION TO	STATI ON	LOCATI ON	LF	REMARKS
0010	35+42 -	35+92	USH 10, RT	50	
0010	36+60 -	36+80	USH 10, RT & LT	50	STORM SEWER REPLACEMENT
0010	38+90 -	39+10	USH 10, RT & LT	50	CULVERT REPLACEMENT
0010	42+09 -	42+59	USH 10, RT	50	
0010	35' WB' + 01 -	35' WB' +51	USH 10, LT	50	
0010	43+46 -	43+96	USH 10, LT	50	
0010	56+63 -	57+13	USH 10, RT	50	
0010	61+01	61+51	USH 10, RT	50	
0010	56+80 -	57+30	USH 10, LT	50	
0010	58+80 -	59+00	USH 10, RT & LT	50	CULVERT REPLACEMENT
0010	61+26 -	61+76	USH 10, LT	50	
0010	72+39 -	72+89	USH 10, RT	50	
0010	73+70 -	73+90	USH 10, RT & LT	50	CULVERT REPLACEMENT
0010	73+83 -	74+33	USH 10, RT	50	
0010	72+50 -	73+00	USH 10, LT	50	
0010	74+81 -	75+31	USH 10, LT	50	
			TOTAL 0010	800	

REMOVING MASONRY

CATEGORY	STATI ON	LOCATI ON	204. 0185 CY	REMARKS	
0010	73+80	USH 10, RT	1	CULVERT PIPE HEADWALL	_
		TOTAL 0010	1		

REMOVING STORM SEWER (15-INCH)

			204. 0245
CATEGORY	STATION TO STATION	LOCATI ON	LF
0010	36+70	USH 10, EB	60
		TOTAL 0010	60

FINISHING ROADWAY (PROJECT)

CATEGORY	LOCATI ON	213. 0100 EACH
0010	PROJECT LIMITS	1
	TOTAL 0010	1

BASE AGGREGATE DENSE 1 1/4-INCH

CATEGORY	STATI ON TO STATI ON	LOCATI ON	305. 0120 TON	REMARKS
0010	36+60 - 36+80	USH 10	30	STORM SEWER REPLACEMENT
0010	38+90 - 39+10	USH 10	85	CULVERT REPLACEMENT
0010	58+80 - 59+00	USH 10	85	CULVERT REPLACEMENT
0010	73+70 - 73+90	USH 10	85	CULVERT REPLACEMENT
		TOTAL 001	10 285	

BASE AGGREGATE DENSE 3/4-INCH

CATEGORY	STATI ON TO STATI ON	LOCATI ON	305. 0110 TON	REMARKS
0010 0010	38+01 - 87+97 87+97 - 118+20	USH 10, RT & LT USH 10, RT & LT	490 680	MAINLINE, SHOULDERS, SIDEROADS, DRIVEWAYS MAINLINE, SHOULDERS, SIDEROADS, DRIVEWAYS
0010	70+50	USH 10, RT & LT	20	DRI VEWAYS
		TOTAL 0010	1190	

PROJECT NO: 1520-04-71	HWY: USH 10	COUNTY: TREMPEALEAU	MISCELLANEOUS QUANTITIES	SHEET:	Е
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FILE NAME : N:\PDS\...\030200_mq.pptx PLOT BY : A.R.H. PLOT NAME : PLOT NAME : PLOT SCALE : 1:1

SHAPING SHOULDERS

 CATEGORY
 STATI ON TO STATI ON STATION
 LOCATI ON STA

 0010
 38+00 - 118+20
 USH 10 RT & LT 162

 TOTAL 0010
 162

DRILLED TIE BARS

			416. 0610	
CATEGORY	STATION TO STATION	LOCATI ON	EACH	REMARKS
0010	6+75 - 7+75	USH 10 WB	52	I-94 WB ON/OFF RAMP INTERSECTION
0010	7+75 - 16+75	USH 10 EB	32	I-94 WB ON RAMP TO HILLTOP RD
0010	5+00 - 38+10	USH 10 EB, WB	30	UNDI STRI BUTED
		TOTAL 0010	114	

DRILLED DOWEL BARS

			416. 0620	
CATEGORY	STATION TO STATION	LOCATI ON	EACH	REMARKS
0010	7+75 - 16+75	USH 10 WB	17	I-94 WB OFF RAMP TO HILLTOP RD
0010	16+75 - 17+75	USH 10 WB	72	I-94 WB ON RAMP TO TRU-GAS RD
0010	17+75 - 27+00	USH 10 WB	8	TRU-GAS RD / HILLTOP RD INTER
0010	5+00 - 38+10	USH 10 WB, EB	10	UNDI STRI BUTED
		TOTAL 0010	107	

TACK COAT

CATEGORY	STATION TO STATION	LOCATI ON	455. 0605 GAL	REMARKS	
0010 0010	38+10 - 87+97 87+97 - 118+20	USH 10 USH 10	3615 1360		(AS DIRECTED BY ENGINEER) (AS DIRECTED BY ENGINEER)
		TOTAL 0010	4975		

ASPHALTIC SURFACE

		465. 0105	
CATEGORY	LOCATI ON	TON	REMARKS
0010	UNDI STRI BUTED	500	LEVELI NG, WEDGI NG
	TOTAL 0010	500	

ASPHALTIC SURFACE PATCHING

CATEGORY	STATI ON	LOCATI ON	465. 0110 TON			REMARKS			
0010 0010	36+70	USH 10 EB RT UNDI STRI BUTED	3 47	SHOULDER POT HOLES,	POP-OUTS,	BUTT JOINTS,	SHOULDERS	NEXT	TO PCC
		TOTAL 0010	50						

ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES

CATEGORY	STATI ON	LOCATI ON	465. 0120 TON	REMARKS
0010	98+40	USH 10, RT	5	DRI VEWAY APRON
0010	103+40	USH 10, LT	5	DRI VEWAY APRON
0010	116+30	USH 10, RT	5	DRI VEWAY APRON
0010	70+50	USH 10, RT & LT	10	DRI VEWAY APRON
		TOTAL 0010	25	

APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 15-INCH

522. 1015

				CATEGORY	STATI ON	LOCATI ON	EACH	REMARKS
	CULVERT PIPE REINFORCED CONCRETE CLASS III 30-INCH			0010	36+70	USH 10 EB RT	1	STORM SEWER PIPE
CATEGORY	STATI ON TO STATI ON	LOCATI ON	522. 0130 LF			TOTAL 0010	1	

120

120

			522. 1030	
CATEGORY	STATI ON	LOCATI ON	EACH	
0010	73+80	USH 10, RT & LT	2	
		TOTAL 0010	2	

APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 30-INCH

CULVERT PIPE REINFORCED CONCRETE CLASS III 42-INCH

73 + 80

USH 10

TOTAL 0010

			522. 0142
CATEGORY	STATION TO STATION	LOCATI ON	LF
			_
0010	39+00	USH 10	170
0010	58+90	USH 10	170
		TOTAL	0010 340

0010

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				522. 1042			STEEL PLATE BEAM GUARD	CLASS A	
CATEGORY	STAT	T ON	LOCATI ON	ЕАСН					
								614. 0305	
0010	39+00	1	USH 10, RT & LT	2	CATEGORY	STATION TO STATION	LOCATI ON	LF	REMARKS
0010	58+90	1	USH 10, RT & LT	2					
					0010	35+35.00 - 35+92.00	USH 10, RT	57. 0	
			TOTAL 0010	4	0010	36+60 - 36+80	USH 10, RT & LT	50	STORM SEWER REPLACEMENT
					0010	42+09.00 - 42+10.00	USH 10, RT	1.0	
					0010	35' WB' +46. 87 - 35' WB' +51	USH 10, LT	4. 1	
					0010	38+90 - 39+10	USH 10, RT & LT	50	CULVERT REPLACEMENT
					0010	43+46. 00 - 44+03. 13	USH 10, LT	57. 1	
					0010	56+09. 37 - 57+13. 00	USH 10, RT	103. 6	
					0010	58+80 - 59+00	USH 10	50	CULVERT REPLACEMENT
					0010	61+01.00 - 61+03.13	USH 10, RT	2. 1	
					0010	55+44. 37 - 57+30. 00	USH 10, LT	185. 6	
			RI PRAP MEDI UM		0010	61+26. 00 - 62+88. 13	USH 10, LT	162. 3	
					0010	71+21. 87 - 72+89. 00	USH 10, RT	167. 1	
					0010	73+70 - 73+90	USH 10	50	CULVERT REPLACEMENT
				606. 0200	0010	73+83. 00 - 76+03. 13	USH 10, RT	220. 1	
CATEO	GORY	STATI ON	LOCATI ON	CY	0010	72+46. 87 - 73+00. 00	USH 10, LT	53. 1	
					0010	74+81. 00 - 76+78. 13	USH 10, LT	197. 1	
001	10	18+00	USH 10, LT	2			TOTAL 0010	1410.0	
001	10	30+20	USH 10, RT	4			TOTAL 0010	1410. 2	
001	10	39+00	USH 10, RT & LT	11					
001	10	58+90	USH 10, RT & LT	14					
001	10	73+80	USH 10, RT & LT	9					
001			UNDI STRI BUTED	10					
			TOTAL 0010	50			STEEL PLATE BEAM GUAR	D ENERGY ABSOI	RBING TERMINAL

			614. 0370
CATEGORY	STATION TO STATION	LOCATI ON	EACH
0010	34+81. 87 - 35+35. 00	USH 10, RT	1
0010	42+10.00 - 42+63.13	USH 10, RT	1
0010	34' WB' +96. 87 - 35' WB' +46. 87	USH 10, LT	1
0010	44+03. 13 - 44+53. 13	USH 10, LT	1
0010	55+59. 37 - 56+09. 37	USH 10, RT	1
0010	61+03. 13 61+53. 13	USH 10, RT	1
0010	54+94. 37 - 55+44. 37	USH 10, LT	1
0010	62+88. 13 - 63+38. 13	USH 10, LT	1
0010	70+71. 87 - 71+21. 87	USH 10, RT	1
0010	76+03. 13 - 76+53. 13	USH 10, RT	1
0010	71+96. 87 - 72+46. 87	USH 10, LT	1
0010	76+78. 13 - 77+28. 13	USH 10, LT	1
		TOTAL 001	0 12

STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 15-INCH

			608. 0415	
CATEGORY	STATI ON	LOCATI ON	LF	
0010	36+70	USH 10, EB	60	
		TOTAL 0010	60	

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	2

BARRI ER	SYSTEM	GRADI NG	SHAPI NG	FI NI SHI NG
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			614. 0010	EXP 1. 25 *FI LL	*SALVAGED TOPSOIL	*MULCHI NG	*FERTI LI ZER TYPE B	*SEEDI NG MI XTURE
CATEGORY.	CTLATE ON TO CTLATE ON	T OCHTE ON						
CATEGORY	STATION TO STATION	LOCATI ON	EACH	CY	SY	SY	СШТ	NO. 20 LBS
0010	34+81. 87 - 35+35. 00	USH 10, RT	1	17	220	220	0. 14	6. 0
0010	42+10.00 - 42+63.13	USH 10, RT	1	0	10	10	0. 01	0. 3
0010	34' WB' +96. 87 - 35' WB' +46. 87	USH 10, LT	1	2	40	40	0. 03	1. 1
0010	44+03. 13 - 44+53. 13	USH 10, LT	1	12	100	100	0. 03	2. 7
0010	55+59. 37 - 56+09. 37	USH 10, RT	1	0	10	10	0. 01	0. 3
0010	61+03. 13 61+53. 13	USH 10, RT	1	0	10	10	0. 01	0. 3
0010	54+94. 37 - 55+44. 37	USH 10, LT	1	64	250	250	0. 14	6. 8
0010	62+88. 13 - 63+38. 13	USH 10, LT	1	35	220	220	0. 14	6. 0
0010	70+71. 87 - 71+21. 87	USH 10, RT	1	0	10	10	0. 01	0. 3
0010	76+03. 13 - 76+53. 13	USH 10, RT	1	5	60	60	0. 04	1. 6
0010	71+96. 87 - 72+46. 87	USH 10, LT	1	3	120	120	0. 08	3. 2
0010	76+78. 13 - 77+28. 13	USH 10, LT	1	0	10	10	0. 01	0. 3
		TOTAL 0010	12					

*ESTIMATED QUANTITIES FOR INFORMATION ONLY. MINOR COMMON QUANTITIES NOT SHOWN

ADDITIONAL FINISHING QUANTITIES (INCLUDED WITH THIS ITEM) MAY BE REQ'D IF SITES ARE SUBSTANTIALLY DISTURBED DURING GUARDRAIL REMOVAL.

MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT)

618. 0100
EACH
_
1
. 0010 1

REPLACING GUARDRAIL POSTS AND BLOCKS

		614. 0950
CATEGORY	LOCATI ON	EACH
0010	UNDI STRI BUTED	100
	TOTAL 0010	100

ADJUSTING STEEL PLATE BEAM GUARD

		614. 0400
CATEGORY	LOCATI ON	LF
0010	UNDI STRI BUTED	1250
	TOTAL 0010	1250

MOBI LI ZATI ON

			619. 1000
CATEGORY	LOCATI ON		EACH
0010	PROJECT		1
		TOTAL 0010	

SALVAGED TOPSOIL

CATEGORY	STATI ON	LOCATI ON	625. 0500 SY
0010	18+00	USH 10, LT	2
0010	30+20	USH 10, RT	4
0010	36+70	USH 10, RT	50
0010	39+00	USH 10, RT & LT	180
0010	58+90	USH 10, RT & LT	180
0010	73+80	USH 10, RT & LT	180
		TOTAL 0010	596

3

	OTTT	310
MILL	.CHT	N(÷

			627. 0200		SILT FENCE MAINTENANCE
CATEGORY	STATI ON	LOCATI ON	SY		SILI FENCE MAINTENANCE
0010	18+00	USH 10, LT	2		
0010	30+20	USH 10, RT	4	CATECODY	LOCATION
0010	36+70	USH 10, RT	50	CATEGORY	LOCATI ON
0010	39+00	USH 10, RT & LT	50	0010	INDI CEDI DIECE
0010	58+90	USH 10, RT & LT	50	0010	UNDI STRI BUTED
0010	73+80	USH 10, RT & LT	50		
0010		UNDI STRI BUTED	20		TOTAL 0010
		TOTAL 0010	226		

EROSI ON BALES

CATEGORY	STATI ON	LOCATI ON	628. 1104 EACH	REMARKS
0010	39+00	USH 10, RT & LT	20	
0010	58+90	USH 10, RT & LT	20	
0010	73+80	USH 10, RT & LT	20	
0010		PROJECT	20	UNDI STRI BUTED
		TOTAL 0010	80	

SILT FENCE

				628. 1504
CATEGORY	STATI ON TO	STATI ON	LOCATI ON	LF
0010		18+00	USH 10, LT	20
0010		30+20	USH 10, RT	20
0010	33+80 -	35+20	USH 10, RT	140
0010	34+50 -	35+60	USH 10, LT	110
0010		36+70	USH 10, RT	50
0010		39+00	USH 10, RT & LT	260
0010	43+80 -	45+30	USH 10, LT	150
0010	53+80 -	55+50	USH 10, LT	170
0010		58+90	USH 10, RT & LT	260
0010	62+80 -	64+00	USH 10, LT	120
0010	70+80 -	72+60	USH 10. LT	180
0010		73+80	USH 10, RT & LT	260
0010	76+00 -	77+80	USH 10, RT	180
			TOTAL 0010	1920

MOBILIZATIONS EROSION CONTROL

628. 1520

LF

100

100

		628. 1905
CATEGORY	LOCATI ON	EACH
0010	UNDI STRI BUTED	3
	TOTAL 0010	3

MOBILIZATIONS EMERGENCY EROSION CONTROL

		628. 1910
CATEGORY	LOCATI ON	EACH
0010	UNDI STRI BUTED	2
	TOTAL 0010	2

PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU MISCELLANEOUS QUANTITIES SHEET: **E**

EROSION MAT CLASS I TYPE B

CATEGORY	STATI ON	LOCATI ON	628. 2004 SY	REMARKS
0010	39+00	USH 10, RT & LT	180	
0010	58+90	USH 10, RT & LT	180	
0010	73+80	USH 10, RT & LT	180	
0010		PROJECT	100	UNDI STRI BUTED
		TOTAL 0010	640	

FERTILIZER TYPE B

			629. 0210	
CATEGORY	STATI ON	LOCATI ON	СШГ	REMARKS
0010	18+00	USH 10, LT	0. 1	
0010	30+20	USH 10, RT	0. 1	
0010	36+70	USH 10, RT	0. 1	
0010	39+00	USH 10, RT & LT	0. 1	
0010	58+90	USH 10, RT & LT	0. 1	
0010	73+80	USH 10, RT & LT	0. 1	
0010		PROJECT	0. 4	UNDI STRI BUTED

TOTAL 0010

TEMPORARY DITCH CHECKS

CATEGORY	STATI ON	LOCATI ON	628. 7504 LF
0010	39+00	USH 10, RT & LT	40
0010	58+90	USH 10, RT & LT	40
0010	73+80	USH 10, RT & LT	40
0010		UNDI STRI BUTED	70
		TOTAL 0010	190

SEEDING MIXTURE NO. 20

CHECKS	PI PE	CULVERT
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CATEGORY	STATI ON	LOCATI ON	628. 7555 EACH	REMARKS
0010	39+00	USH 10, RT & LT	10	
0010	58+90	USH 10, RT & LT	10	
0010	73+80	USH 10, RT & LT	10	
0010		PROJECT	5	UNDI STRI BUTED
		TOTAL 0010	35	

			630. 0120
CATEGORY	STATI ON	LOCATI ON	LB
0010	18+00	USH 10, LT	0. 1
0010	30+20	USH 10, RT	0. 1
0010	36+70	USH 10, RT	1. 4
0010	39+00	USH 10, RT & LT	5. 0
0010	58+90	USH 10, RT & LT	5. 0
0010	73+80	USH 10, RT & LT	5. 0
		TOTAL 0010	16. 6

SHEET: Ε HWY: USH 10 COUNTY: TREMPEALEAU PROJECT NO: 1520-04-71 MISCELLANEOUS QUANTITIES

FILE NAME: N:\PDS\...\030200_mq.pptx PLOT DATE: June 14, 1911 PLOT BY: A.R.H. PLOT NAME : PLOT SCALE: 1:1

			633. 5200	
CATEGORY	STATI ON	LOCATI ON	EACH	REMARKS
0010	30+20	USH 10, RT	1	
0010	39+00	USH 10, RT & LT	2	
0010	58+90	USH 10, RT & LT	2	
0010	73+80	USH 10, RT & LT	2	
0010		PROJECT	2	UNDI STRI BUTED
		TOTAL 0010	9	

TRAFFIC CONTROL DRUMS

			643. 0300	
CATEGORY	STATION TO STATION	LOCATI ON	DAY	REMARKS
0010	5+00 - 38+10	USH 10	1400	PCC REPAIRS
0010		UNDI STRI BUTED	100	
		TOTAL 0010	1500	

FI ELD OFFI CE TYPE B

MARKERS CULVERT END

			642. 5001
CATEGORY	LOCATI ON		EACH
0010	PROJECT		1
		TOTAL 0010	1

TRAFFIC CONTROL BARRICADES TYPE III

				643. 0420	
	CATEGORY	STATION TO STATION	LOCATI ON	DAY	REMARKS
	0010	5+00 - 38+10	USH 10	112	PCC REPAIRS
	0010	38+10 - 88+00	USH 10	280	CULVERT REPLACEMENTS
TRAFFIC CONTROL (PROJECT) 01. 1520-04	0010		PROJECT	84	DETOUR ROUTE
643.0100			TOTAL 0010	476	

CATEGORY	LOCATI ON		EACH
0010	PROJECT		1
		TOTAL 0010	1

SHEET: Ε HWY: USH 10 COUNTY: TREMPEALEAU PROJECT NO: 1520-04-71 MISCELLANEOUS QUANTITIES PLOT SCALE: 1:1

FILE NAME: N:\PDS\...\030200_mq.pptx PLOT DATE: June 14, 1911 PLOT BY: A.R.H. PLOT NAME :

TRAFFIC CONTROL WARNING LIGHTS TYPE A

				643. 0705	
CATEGORY	STATION TO STATION	LOCATI ON		DAY	REMARKS
•					_
0010	5+00 - 38+10	USH 10		224	PCC REPAIRS
0010	38+10 - 88+00	USH 10		336	CULVERT REPLACEMENTS
			TOTAL 0010	560	

TRAFFIC CONTROL SIGNS

			643. 0900	
CATEGORY	LOCATI ON		DAY	REMARKS
0010	PROJECT		252	PCC REPAIRS
		TOTAL 0010	252	

TRAFFIC CONTROL WARNING LIGHTS TYPE C

					643. 0715	
CATEGORY	STATION T	O STATION	LOCATI ON		DAY	REMARKS
0010	5+00	- 38+10	USH 10		196	PCC REPAIRS
				TOTAL 0010	196	

TRAFFIC CONTROL COVERING SIGNS TYPE II

		643. 0920	
CATEGORY	LOCATI ON	EACH	REMARKS
0010	DETOUD DOUTE	15	CEE DETOUD DIAN EOD LOCATIONS
0010	DETOUR ROUTE	15	SEE DETOUR PLAN FOR LOCATIONS
	TOTAL 0010	15	

TRAFFI C CONTROL DETOUR (PROJECT) 01. 1520-04-71

			643. 2000
CATEGORY	LOCATI ON		EACH
0010	PROJECT		1
		TOTAL 0010	1

TRAFFIC CONTROL ARROW BOARDS

				643. 0800	
CATEGORY	STATION TO STATION	LOCATI ON		DAY	REMARKS
0010	5+00 - 38+10	USH 10		56	PCC REPAIRS
			TOTAL 0010	56	

TRAFFIC CONTROL DETOUR SIGNS

			643. 3000
CATEGORY	LOCATI ON		DAY
0010	PROJECT		2982
		TOTAL 0010	2982

EAST BOUND SIGN SUMMARY

											634.0616 POST WOOI		637.2215 SIGNS TYPE I		638.3000 REMOVING	638.2602
САТ	SIGN NO	STATION	DIR	SIGN CODE	WIDTH (IN)	HEIGHT (IN)	DESCRIPTION	SIGN MESSAGE LINE ONE	SIGN MESSAGE LINE TWO	SIGN MESSAGE LINE THREE	4X6 INCH X 16 FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	REFLECTIVE H FOLDING, SF	637.223 SIGNS TYPE II REFLECTIVE F SF	SMALL SIGN SUPPORTS EACH	REMOVING SIGNS TYPE II EACH
0010	1	5+47.99	_	J3-1	24	57	ROUTE TURN ASSEMBLY(1 HEADED ROUTE PANEL)	WEST	94	[LA]	1	9.06	·			3
				MB3-4	24	12	WEST -BLUE CARDINAL ROUTE									
				M1-1	24	24	INTERSTATE SHIELD									
				MB6-1	21	21	ARROW- LEFT									
0010	2	6+62.53	EB	R4-7	24	30	KEEP RIGHT				1	5			1	1
0010	3	6+91.25	EB	J3-1	24	57	ROUTE TURN ASSEMBLY(1 HEADED ROUTE PANEL)	WEST	94	[LA]		9.06				3
				M1-1	24	12	WEST-BLUE CARDINAL ROUTE									
				MB3-4	24	24	INTERSTATE SHIELD									
				MB6-1	21	21	ARROW- LEFT									
0010	4	7+03.94	EB	R3-1	36	36	NO RIGHT TURN					9				1
0010	4A	7+87.97	_	R5-1	36	36	DO NOT ENTER				1	9			1	1
0010	5	8+00.57		R4-7	24	30	KEEP RIGHT					5				1
0010	6	7+05.00	_	R6-2-L	30	36	ONE WAY LEFT ARROW					7.5				1
0010	7	8+38.93	_	R6-2-R	30	36	ONE WAY RIGHT ARROW					7.5				1
0010	8	9+86.65	_	J1-1	24	36	REASSURANCE ASSEMBLY (1 HEADED ROUTE PANEL)	EAST	10		1	6			1	2
0010	0	3180.03	LD	M3-2	24	12	EAST CARDINAL ROUTE MARKER	LAST	10		1	0			1	2
				M1-4	24	24	US ROUTE MARKER									
-	9	10.20	EB	E10-82		REMAINS	TOURIST-ORIENTED DIRECTIONAL SIGNS (TODS)									
-	9	10+20	EB		REMAINS	REIVIAINS	,									
2010	- 10	44.75.00		ASIS	20	26	SUPER 8 MOTEL AND STONY CREEK	DOVICEOUTE OF ALAFRICA	TD000 400							
0010	10	11+75.08		I-55-56	30	36	ADOPT-A-HIGHWAY	BOY SCOUTS OF AMERICA	TROOP 102		1	7.5			1	1
0010	12	12+39.52		R2-1	30	36	SPEED LIMIT _ MPH	40			1	7.5			1	1
0010	13	16+66.07	_		36	36	NO U-TURN SYMBOL				1	9			1	1
0010	14	16+66.07	_	R1-1-F	36	36	STOP (FOLDING)				1		7.46		1	1
0010	14A	17+00	EB	R6-2L	30	36	ONE WAY LEFT ARROW				1	7.5			1	1
0010	15	18+09.25	_		24	30	KEEP RIGHT					5				1
0010	16	18+09.25	EB	R3-4	36	36	NO U-TURN SYMBOL					9				1
0010	17	18+09	EB	R5-1	36	36	DO NOT ENTER				1	9			1	1
0010	18	17+89.41	. EB	R1-1-F	36	36	STOP (FOLDING)						7.46			1
0010	19	17+89.41	. EB	R6-2-R	30	36	ONE WAY RIGHT ARROW				1	7.5			1	1
0010	20	17+89.41	. EB	R6-3	30	24	DIVIDED HIGHWAY CROSSING SIGN AND INTERSECTION				1	5			1	1
0010	21	19+74.52	EB	J4-2	48	36	REASSURANCE ASSEMBLY (2 HEADED ROUTE PANEL)	EAST, BLANK	10, R		1	12			1	1
				M3-2	24	12	EAST CARDINAL ROUTE MARKER									
				M1-4	24	24	US ROUTE MARKER									
					24	12	BLANK									
				M1-5A	24	24	COUNTY MARKER									
0010	22	21+50.44	EB	W9-1-L	36	36	LEFT LANE ENDS				1			9	1	1
0010	23	23+16.35	EB	R2-1	30	36	SPEED LIMIT MPH	55			1	7.5			1	1
0010	24	23+16.32	_		72	24	DESTINATION/DISTANCE (TWO) WITH DIE CUT LETTERS	Augusta 10	Fairchild 13		1	12			1	1
0010	25	24+99.52		W4-2-L	36	36	LANE REDUCTION TRANSITION SYMBOL- (LEFT LANE)	I sagares =			1			9	1	1
0010	26	25+06.93		W4-2-L	36	36	LANE REDUCTION TRANSITION SYMBOL- (LEFT LANE)		<u> </u>		1		<u> </u>	9	1	1
0010	27	28+97.93	_	R5-1	36	36	DO NOT ENTER				1	9		 	1	1
0010		28+22.09		<u> </u>	24	30	KEEP RIGHT				1	5		+	1	1
0010	29	27+10.00			30	36	ONE WAY LEFT ARROW				1	7.5	1	+	1	1
0010	30	41+52.17			36	36	TWO-WAY TRAFFIC SYMBOL		1		1	1.3	1	9	1	
					1				+				+			1
0010	31	69+81.31			48	36	NO PASSING ZONE		-		1	6.5	-	12	1	1
0010	32	75+04.84	FR		24	39	DIRECTIONAL WITHOUT CARDINAL (1 HEADED ROUTE PANEL)		R		1	6.5			1	1
				M2-1	21	15	JUNCTION MARKER									
		-		M1-5A	24	24	COUNTY MARKER									
0010	33	81+00	_		36	36	RIGHT CURVE				1			9	1	1
0010	34	83+00			66	30	TWO DESTINATIONS (ARROWS)	[LA] Augusta	Fairchild [TR]			13.75				
0010	35	86+51.56	EB		24	45	DIRECTIONAL WITHOUT CARDINAL (1 HEADED ROUTE PANEL)	R	[LA]		1	7.5			1	1
				M1-5A	24	24	COUNTY MARKER									
				M6-1	21	21	ARROW-LEFT									
0010	36	86+80.29	_		60	30	NIGHT ARROW (DOUBLE)				1			12.5	1	1
0010	37	92+00.88			36	36	STOP				1	7.46			1	1
0010	38	109+77.38	8 EB	I-55-56	30	36	ADOPT-A-HIGHWAY	OSSEO-FAIRCHILD	NATIONAL	HONOR SOCIETY		7.5			1	1
										Total=	27	220.77	14.92	69.50	28	39
															·	· · · · · · · · · · · · · · · · · · ·

PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU MISCELLANEOUS QUANTITIES SHEET: **E**

WEST BOUND SIGN SUMMARY | 634.0616 POST | 637.2210 SIGNS | 637.2215 SIGNS |

											634.0616 POST	637.2210 SIGNS	637.2215 SIGNS	627 2220 CICNIC	638.3000 REMOVING SMALL	630 3603	
	SIGN								SIGN MESSAGE	SIGN MESSAGE	WOOD 4X6 INCH X 16 FT	TYPE II REFLECTIVE H	TYPE II REFLECTIVE H	637.2230 SIGNS TYPE II	SIGN SUPPORTS	638.2602 REMOVING SIGNS	
			_		, ,	HEIGHT (IN)	DESCRIPTION	SIGN MESSAGE LINE ONE	LINE TWO	LINE THREE	EACH	SF	FOLDING, SF	REFLECTIVE F SF	EACH	TYPE II EACH	REMARKS
0010		118+33.48	_	1-2-2	90	15	COUNTY LINE NAME	Trempealeau Co	TD00D 103		1	9.375			1	1	
0010 0010	76	117+30.14 107+75		I-55-56 W1-2-L	30 36	36 36	ADOPT-A-HIGHWAY LEFT CURVE	BOY SCOUTS OF AMERICA	TROOP 102		1	7.5		9	1	1	
0010		108+58.70		R1-1	30	30	STOP				1	5.18		<u>J</u>	1	1	
0010	74	107+97.50	WB	W14-3	48	36	NO PASSING ZONE				1			12	1	1	
0010	73	98+79.40	WB	J1-1	24	39	REASSURANCE ASSEMBLY (1 HEADED ROUTE PANEL)		R		1	6.5			1	1	
				M2-1	21	15	JUNCTION MARKER										
0010	72	92+41.04	W/R	M1-5A D1-1	24 66	24 15	COUNTY MARKER ONE DESTINATION (ARROW)	Augusta (RA)				6.875					
		88+47.00	_	J13-1	24	45	DIRECTIONAL WITHOUT CARDINAL (1 HEADED ROUTE PANEL)	R	[RA]		1	7.5			1	1	
				M1-5A	24	24	COUNTY MARKER										
				M6-1	21	21	ARROW- RIGHT										
		86+97.10	-	R1-1	30	30	STOP	DIANK TO	10.04	[00] [00]	1	5.18			1	1	
0010	68	86+93.53	WB	J3-2	48 24	57 12	DIRECTIONAL ASSEMBLY (2 HEADED ROUTE PANEL) BLANK	BLANK, TO	10, 94	[DBA] [RA]	1	19			1	1	
				M1-4	24	24	US ROUTE MARKER										
				M6-4	21	21	DIRECTION ARROW LEFT - RIGTH										
				MB4-5	24	14	ТО										
				M1-1	24	24	INTERSTATE SHIELD										
2017		45.05		MB6-1	21	21	ARROW-RIGHT										
0010 0010		46+35.26		R10-64 W14-3	30	36 36	NO ENGINE BRAKING EXCEPT IN EMERGENCY				1	7.5		12	1	1	
0010		44+89.58 44+52.66	-	W14-3 W6-1	48 36	36 36	NO PASSING ZONE DIVIDED HIGHWAY AHEAD SYMBOL				1 1			12 9	1	1	
0010		36+65.61	_	R5-1	36	36	DO NOT ENTER				1	9			1	1	
0010	63	36+64.10	WB	R4-7	36	48	KEEP RIGHT					12				1	
0010		36+64.10	_	W5-54	18	18	CLEARANCE MARKER WITH YELLOW TYPE F SHEETING				1			2.25	1	1	
0010		30+92.99	_	SP	REMAINS	REMAINS	ANY SPECIAL SIGN OSSEO INDUSTRIAL PARK SIGN										NOT SHOWN ON PLAN
0010 0010		28+97.93 27+10.00		W3-5 R5-1	36 36	36 36	SPEED LIMIT _ AHEAD [ARROW] DO NOT ENTER	40				9		9		1	
		27+10.00		R1-1-F	36	36	STOP (FOLDING)					9	7.46			1	
		27+17.12	_	R6-3	30	24	DIVIDED HIGHWAY CROSSING SIGN AND INTERSECTION					5	7.40			1	
0010		27+80.00	_	R6-2-L	30	36	ONE WAY LEFT ARROW				1	7.5			1	1	
0010		26+82.04	WB	R4-7	24	30	KEEP RIGHT				1	5			1	1	
	54		WB	E10-82	REMAINS	REMAINS	TOURIST-ORIENTED DIRECTIONAL SIGNS (TODS)								_		
-		23+17.27 20+97.68	-	R2-1 W3-3	30 36	36 36	SPEED LIMIT _ MPH SIGNAL AHEAD	40			1	7.5		9	1	1	
-		16+66.00	-	R5-1	36	36	DO NOT ENTER				1	9		9	1	1	
-		17+78.12	_	R6-2-L	30	36	ONE WAY LEFT ARROW				1	7.5			1	1	
0010	49A	16+95.33	WB	R1-1-F	36	36	STOP (FOLDING)						7.46				SHARES POST WITH 49
0010		16+95.33	_	R6-3	30	24	DIVIDED HIGHWAY CROSSING SIGN AND INTERSECTION				1	5			1	1	
0010	-	18+09.00	WB	R1-1-F	36	36	STOP (FOLDING)						7.46			1	
0010 0010	47 46	16+66.02 16+09.92	WB	R4-7 J1-1	24 24	30 39	KEEP RIGHT REASSURANCE ASSEMBLY (1 HEADED ROUTE PANEL)	JCT	94		1	5 6.5			1	2	
0010	40	10103.32	VVD	MB2-1	21	15	JCT - BLUE	301	54		1	0.5			1	2	
				M1-1	24	24	INTERSTATE SHIELD										
		14+70.97		R2-1	30	36	SPEED LIMIT _ MPH	40			1	7.5			1	1	
0010	44	13+34.75	WB	J2-3	72	57	ROUTE TURN ASSEMBLY (3HEADED ROUTE PANEL)	EAST WEST WEST	94-10-94	[UA] [UA] [RT]	1	28.5			1	1	
				MB3-2 M1-1	24 24	12 24	EAST-BLUE INTERSTATE SHIELD										
				MB6-1	24	24	ARROW- AHEAD										
				M3-4	24	12	WEST										
				M1-4	24	24	US ROUTE MARKER										
				M6-1	21	21	ARROW- AHEAD										
				MB3-4	24	12	WEST-BLUE										
				M1-1 MB5-1R	24 21	24 21	INTERSTATE SHIELD ARROW-ADVANCED RIGHT TURN										
0010	43	12+39.52	WB	R5-1	36	36	DO NOT ENTER					9				1	SHARES POST WITH 11
		10+92.08	_	D1-3	72	36	TRIPLE DESTINATION/ARROW	[UA] OSSEO	[UA] MADISON	EAU CLAIRE [RA]	1	18			1	1	
0010	41	8+00.64	_	R3-2	36	36	NO LEFT TURN SYMBOL					9				1	
0010	40	7+87.97	WB	J3-1	24	57	ROUTE TURN ASSEMBLY(1 HEADED ROUTE PANEL)	WEST	94	[RA]		9.5				3	SHARES POST WITH 4A
				MB3-4 M1-1	24 24	12 24	WEST - BLUE INTERSTATE SHIELD										
				MB6-1	24	21	ARROW- RIGHT										
0010	39A	7+03.94	WB	R5-1	36	36	DO NOT ENTER					9.00				1	SHARES POST WITH 4
-		6+20.00		R2-1	30	36	SPEED LIMIT_MPH	40			1	5			1	1	
									, 	Subtotal=	26	258.11	22.38	62.25	27	41	
										Total=	53	478.88	37.3	131.75	55	80	

GEOTEXTILE FABRIC TYPE HR

CATEGORY	STATI ON	LOCATI ON	645. 0120 SY
0010	18+00	USH 10, LT	1
0010	30+20	USH 10, RT	2
0010	39+00	USH 10, RT & LT	57
0010	58+90	USH 10, RT & LT	57
0010	73+80	USH 10, RT & LT	57
		T0TAL 0010	174

PAVEMENT MARKING SAME DAY EPOXY 4-INCH

CATEGORY	STATI ON TO STATI ON	LOCATI ON	646. 0406 LF	REMARKS
0010	38+10 - 118+20	USH 10	9569	CENTERLI NE
		TOTAL 0010	9569	

PAVEMENT MARKING EPOXY 4-INCH

			646. 0106	
CATEGORY	STATION TO STATION	LOCATI ON	LF	REMARKS
0010	38+10 - 118+20	USH 10, RT	7883	WHITE EDGELINE
0010	38+10 - 118+20	USH 10, LT	7843	WHITE EDGELINE
0010		UNDI STRI BUTED	100	PCC REPAIRS
		TOTAL 0010	15826	
		TOTAL OUTO	10020	

PAVEMENT MARKING EPOXY 8-INCH

			646. 0126	
CATEGORY	STATION TO STATION	LOCATI ON	LF	REMARKS
0010	52+90 - 54+40	USH 10 , LT	247	TURNLANE RI DGEVI EW DR
0010	85+25 - 86+65	USH 10 , RT	140	TURNLANE CTH R
0010	87+14 - 90+12	USH 10, LT	238	TURNLANE CTH R
		TOTAL 0010	625	

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PAVEMENT MARKING DIAGONAL EPOXY 12-INCH

LOCATI ON

USH 10

USH 10

TOTAL 0010

CATEGORY STATION TO STATION

0010 80+00 - 85+40
0010 87+85 - 93+05

PAVEMENT MARKING ARROWS EPOXY TYPE 2

REMARKS

PCC REPAIRS

CATEGORY	STATION TO STATION	LOCATI ON	647. 0166 EACH	REMARKS
0010	85+25 - 86+65	USH 10, RT	2	LEFT TURNLANE CTH R
		TOTAL 0010	2	

646. 0600

LF

175

REMOVING PAVEMENT MARKINGS

TOTAL 0010

LOCATI ON

UNDI STRI BUTED

CATEGORY

0010

LOCATING NO-PASSING ZONES

CATEGORY	STATION TO STATION	LOCATI ON	648. 0100 <u>MI</u>
0010	38+10 - 118+20	USH 10	1. 52
		TOTAL	0010 1. 52

PAVEMENT MARKING WORDS EPOXY

CATEGORY	STATION TO STATION	LOCATI ON	647. 0356 EACH	REMARKS	
CATEGORI	STATION TO STATION	LOCATION	LACII	NEWHINS	
0010	85+25 - 86+65	USH 10, RT	1	LEFT TURNLANE CTH R	

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 3

647. 0726

LF

190

450

TEMPORARY PAVEMENT MARKING 4-INCH

CATEGORY	STATI ON TO STATI ON	LOCATI ON	649. 0100 LF	REMARKS
0010	38+10 - 118+20	USH 10	9569	CENTERLINE HMA 1ST LIFT
0010	38+10 - 118+20	USH 10	9569	CENTERLINE HMA 2ND LIFT
		TOTAL O	010 19138	

CONSTRUCTION STAKING PIPE CULVERTS

TOTAL 0010

			650. 6000
CATEGORY	STATI ON	LOCATI ON	EACH
0010	39+00	USH 10	1
0010	58+90	USH 10	1
0010	73+80	USH 10	1

TEMPORARY PAVEMENT MARKING REMOVABLE TAPE 4-INCH

		649. 0400	
CATEGORY	LOCATI ON	LF	REMARKS
0010	UNDI STRI BUTED	1000	PCC REPAIRS
	TOTAL 0010	1000	

CONSTRUCTION STAKING RESURFACING REFERENCE

			650. 8000
CATEGORY	STATION TO STATION	LOCATI ON	LF
0010	5+00 - 118+20	USH 10, EB	11320
0010	4+84 - 40+82	USH 10, WB	3598
		TOTAL 0010	14918

CONSTRUCTION STAKING STORM SEWER

				650. 4000
CATEGORY	STATI ON	LOCATI ON		EACH
0010	36+70	USH 10 EB		2
			TOTAL 0010	2
			TOTAL OUT	۵

PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU MISCELLANEOUS QUANTITIES SHEET: **E**

SAWING ASPHALT

			690. 0150	
CATEGORY	STATI ON	LOCATI ON	LF	REMARKS
0010				
0010	38+10	USH 10	52	MAINLINE BUTTJOINT
0010	38+90	USH 10	53	CULVERT REPLACEMENT
0010	39+10	USH 10	53	CULVERT REPLACEMENT
0010	51+60	USH 10, LT	34	RIDGE VIEW RD JOINT
0010	51+60	USH 10, RT	34	SIDE RD, BUTT JOINT
0010	58+80	USH 10	53	CULVERT REPLACEMENT
0010	59+00	USH 10	53	CULVERT REPLACEMENT
0010	73+70	USH 10	53	CULVERT REPLACEMENT
0010	73+90	USH 10	53	CULVERT REPLACEMENT
0010	87+20	USH 10, LT	50	CTH R BUTTJOINT
0010	87+97	USH 10	62	MAINLINE BUTTJOINT
0010	91+70	USH 10, RT	37	HOGSTAD RD BUTTJOINT
0010	108+80	USH 10, LT	44	SIDE RD BUTTJOINT
0010	118+20	USH 10	30	MAINLINE BUTTJOINT
		TOTAL 0010	661	

SAWING CONCRETE

				690. 025	
CATEGORY	STATI ON T	O STATI ON	LOCATI ON	LF	REMARKS
0010	7+75	16+75	USH 10 WB	70	CONCRETE PAVEMENT REPAIR
0010	16+75	17+75	USH 10 EB	180	CONCRETE PAVEMENT REPAIR
0010	17+75 -	27+00	USH 10 EB/WB	30	CONCRETE PAVEMENT REPAIR
0010	5+00 -	38+10	USH 10	40	UNDI STRI BUTED
0010	6+75	7+75	USH 10 WB	110	36" CENTERLI NE REPAI R
0010	7+75	16+75	USH 10 EB	70	36" CENTERLI NE REPAI R
0010	5+00 -	38+10	USH 10 EB/WB	30	UNDI STRI BUTED
0010	36+60	36+80	USH 10 EB	50	STORM SEWER PIPE
			TOTAL 0010	580	

SPECIAL 02 GRADING, SHAPING AND FINISHING DRIVEWAY REMOVAL 34+60

				*SALVAGED			*FERTI LI ZER	*SEEDI NG		
			SPV. 0105(02)	*FI LL	*WASTE	TOPSOI L	*MULCHI NG	TYPE B	MI XTURE	
CATEGORY	STATI ON	LOCATI ON	LS	(CY)	(CY)	(SY)	(SY)	(CWT)	NO. 20 (LBS)	
									_	
0010	34+60	USH 10, RT	1	0	42	23	106	0.07	2	
		TOTAL 0010	1							

*ESTIMATED QUANTITIES FOR INFORMATION ONLY. MINOR COMMON QUANTITIES NOT SHOWN

ADDITIONAL FINISHING QUANTITIES (INCLUDED WITH THIS ITEM) MAY BE REQ'D IF SITES ARE SUBSTANTIALLY DISTURBED.

SPECIAL 03 GRADING, SHAPING AND FINISHING DRIVEWAY REMOVAL 47+30 LT

						*SALVAGED	*FERTI LI ZER	*SEEDI NG	
			SPV. 0105(03)		*WASTE	TOPSOI L	*MULCHI NG	TYPE B	MI XTURE
CATEGORY	STATI ON	LOCATI ON	LS	(CY)	(CY)	(SY)	(SY)	(CWT)	NO. 20 (LBS)
0010	47+30	USH 10 , LT	1	0	14	18	84	0. 05	2
		TOTAL 0010	1						

*ESTIMATED QUANTITIES FOR INFORMATION ONLY. MINOR COMMON QUANTITIES NOT SHOWN

ADDITIONAL FINISHING QUANTITIES (INCLUDED WITH THIS ITEM) MAY BE REQ'D IF SITES ARE SUBSTANTIALLY DISTURBED.

SPECIAL 04 GRADING, SHAPING AND FINISHING DRIVEWAY REMOVAL 47+30 RT

						*SALVAGED)	*FERTI LI ZER	*SEEDI NG
		SP'	SPV. 0105(04)		*WASTE	TOPSOI L	*MULCHI NG	TYPE B	MI XTURE
CATEGORY	STATI ON	LOCATI ON	LS	(CY)	(CY)	(SY)	(SY)	(CWT)	NO. 20 (LBS)
0010	47+30	USH 10 , RT	1	0	49	34	186	0. 12	4
		TOTAL 0010	<u> </u>						

*ESTIMATED QUANTITIES FOR INFORMATION ONLY. MINOR COMMON QUANTITIES NOT SHOWN

ADDITIONAL FINISHING QUANTITIES (INCLUDED WITH THIS ITEM) MAY BE REQ'D IF SITES ARE SUBSTANTIALLY DISTURBED.

SPECIAL OG GRADING, SHAPING AND FINISHING AND RELOCATING DRIVEWAY 71+50 RT

							*SALVAGED		*FERTI LI ZER	*SEEDI NG
			SPV. 0105(06)		*FI LL	*WASTE	TOPSOI L	*MULCHI NG	TYPE B	MI XTURE
CATEGORY	STATI ON	LOCATI ON	LS	REMARKS	(CY)	(CY)	(SY)	(SY)	(CWT)	NO. 20 (LBS)
0010	71.50	UCH 10 PT	1	MOVE TO CTA GO. TO DT	0	0	05	100	0.07	9
0010	71+50	USH 10, RT	1	MOVE TO STA 70+50 RT	0	0	95	103	0. 07	Z
		TOTAL 0010	1							

*ESTIMATED QUANTITIES FOR INFORMATION ONLY. MINOR COMMON QUANTITIES NOT SHOWN

ADDITIONAL FINISHING QUANTITIES (INCLUDED WITH THIS ITEM) MAY BE REQ'D IF SITES ARE SUBSTANTIALLY DISTURBED.

SPECIAL 05 GRADING, SHAPING AND FINISHING AND RELOCATING DRIVEWAY 71+50 LT

							*SALVAGED		*FERTI LI ZER	*SEEDI NG	
			SPV. 0105(05)		*FI LL	*WASTE	TOPSOI L	*MULCHI NG	TYPE B	MI XTURE	
CATEGOR	Y STATION	LOCATI ON	LS	REMARKS	(CY)	(CY)	(SY)	(SY)	(CWT)	NO. 20 (LBS)	
0010	71+50	USH 10, LT	1	MOVE TO STA 70+50 LT	0	0	107	100	0. 07	2	
		TOTAL 0010) 1								

*ESTIMATED QUANTITIES FOR INFORMATION ONLY. MINOR COMMON QUANTITIES NOT SHOWN

ADDITIONAL FINISHING QUANTITIES (INCLUDED WITH THIS ITEM) MAY BE REQ'D IF SITES ARE SUBSTANTIALLY DISTURBED.

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SPECIAL O1 CONCRETE PAVEMENT REPAIR DOWELED

			SPV. 0180(01)	
CATEGORY	STATION TO STAT	TI ON LOCATI ON	SY	REMARKS
0010	7+75 - 16+75	5 USH 10 WB	30	I-94 WB RAMP TO TRU-GAS RD
0010	16+75 - 17+75	5 USH 10 EB	50	TRU-GAS RD/HILLTOP RD INTER
0010	17+75 - 27+00	USH 10 EB/WB	8	TRU-GAS RD TO INDUSTRIAL
0010	36+60 - 36+80	USH 10	35	STORM SEWER
0010	5+00 - 38+10	USH 10	15	UNDI STRI BUTED
		TOTAL 0010	0 138	

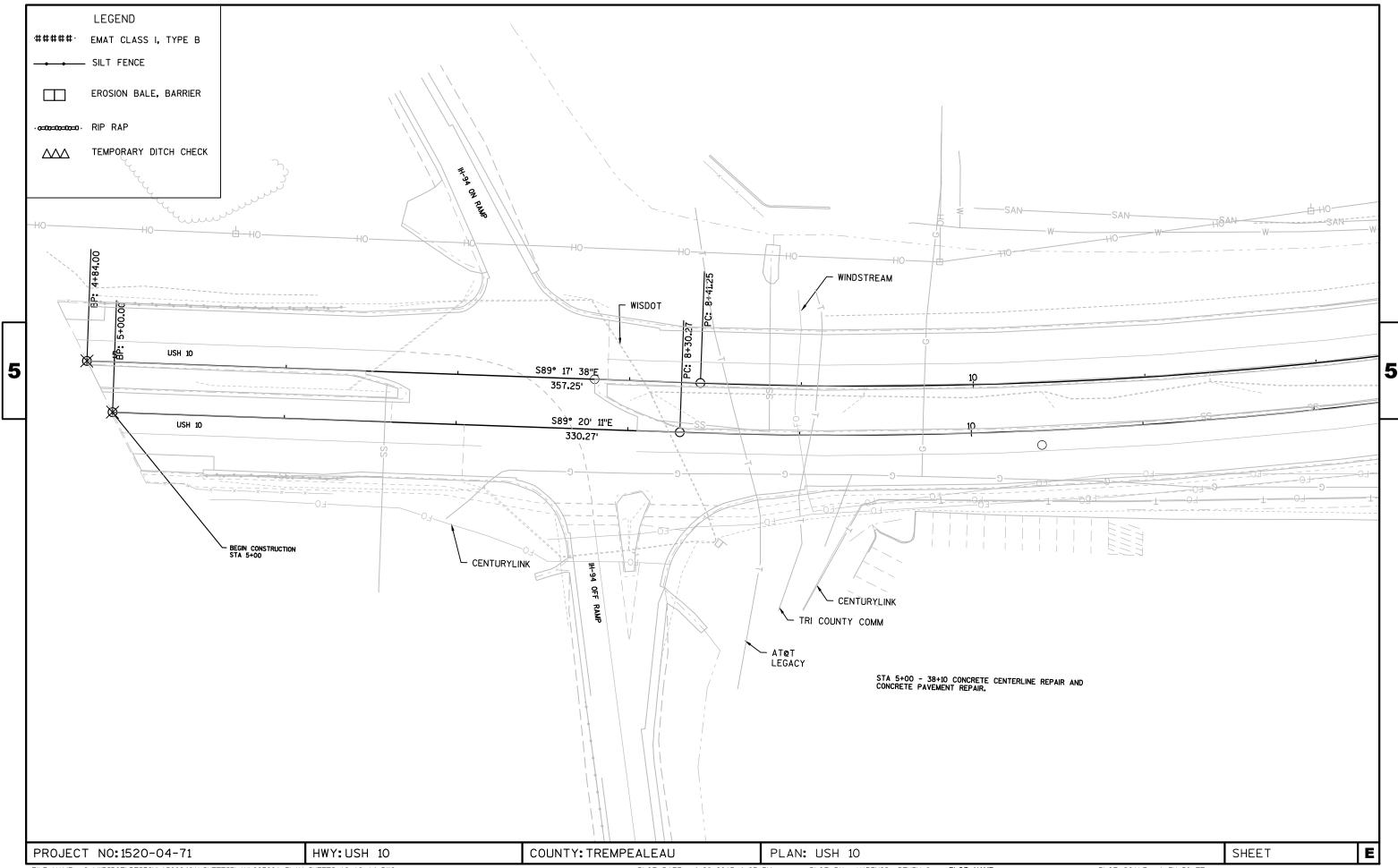
SPECIAL 02 36" CONCRETE CENTERLINE REPAIR

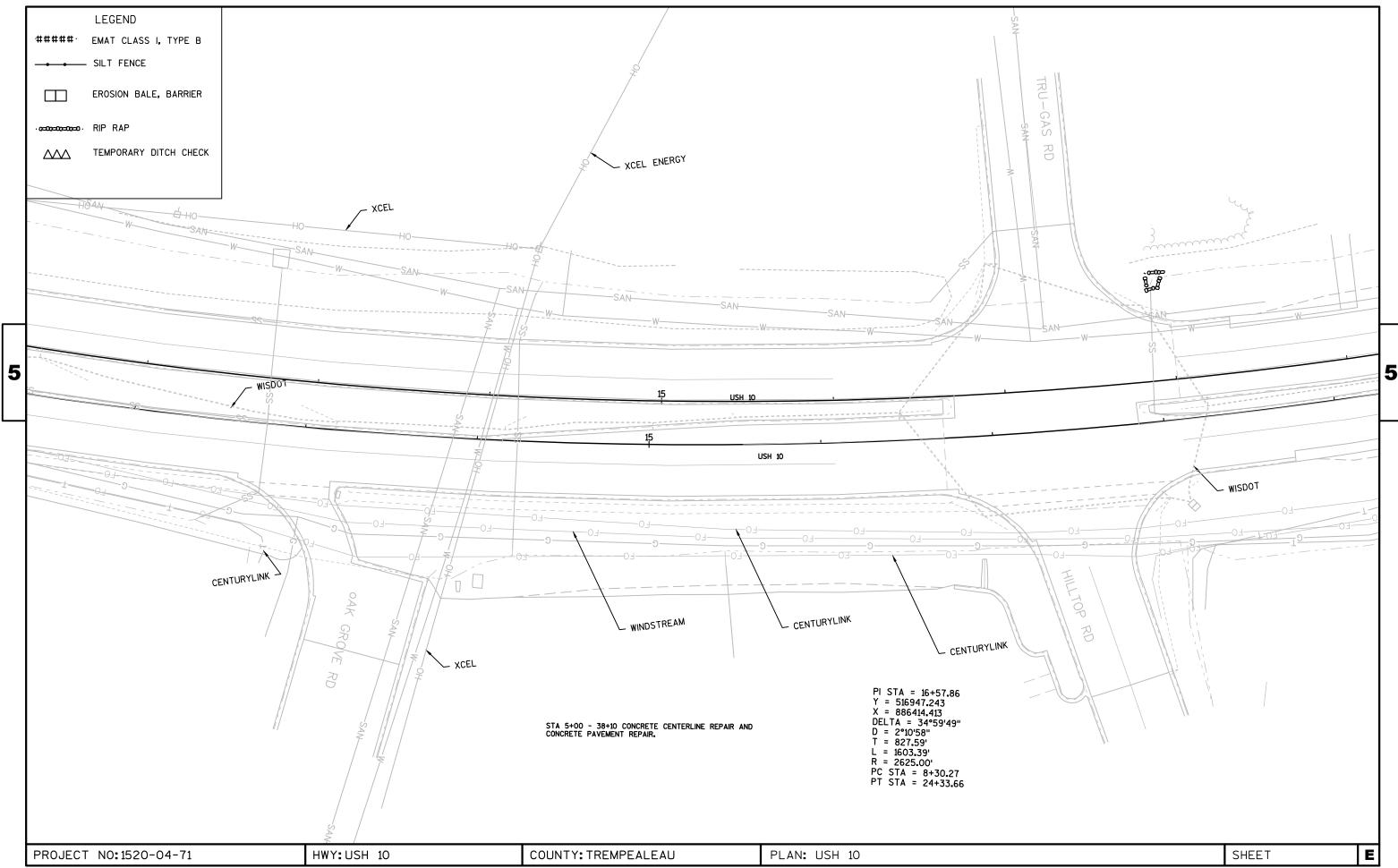
			SPV. 0180(02)	
CATEGORY	STATION TO STATION	LOCATI ON	SY	REMARKS
0010	6+75 - 7+75	USH 10 WB	18	I-94 ON/OFF RAMP INTERSECTION
0010	7+75 - 16+75	USH 10 EB	10	I-94 OFF RAMP TO HILLTOP RD
0010	5+00 - 38+10	USH 10 EB/WB	10	UNDI STRI BUTED
		TOTAL 0010	38	

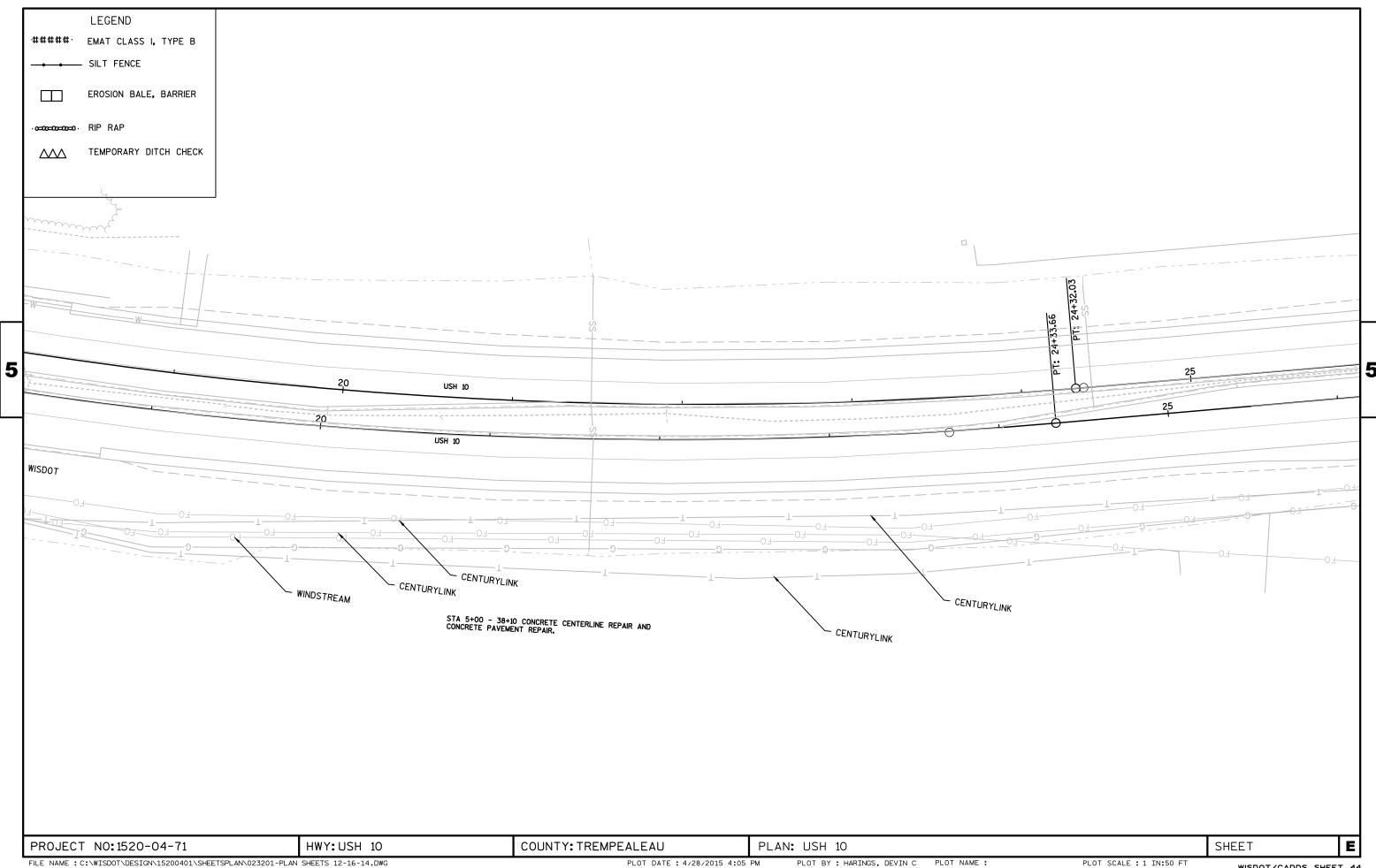
SPECIAL 01 HMA PAVEMENT TYPE E-3 SPECIAL

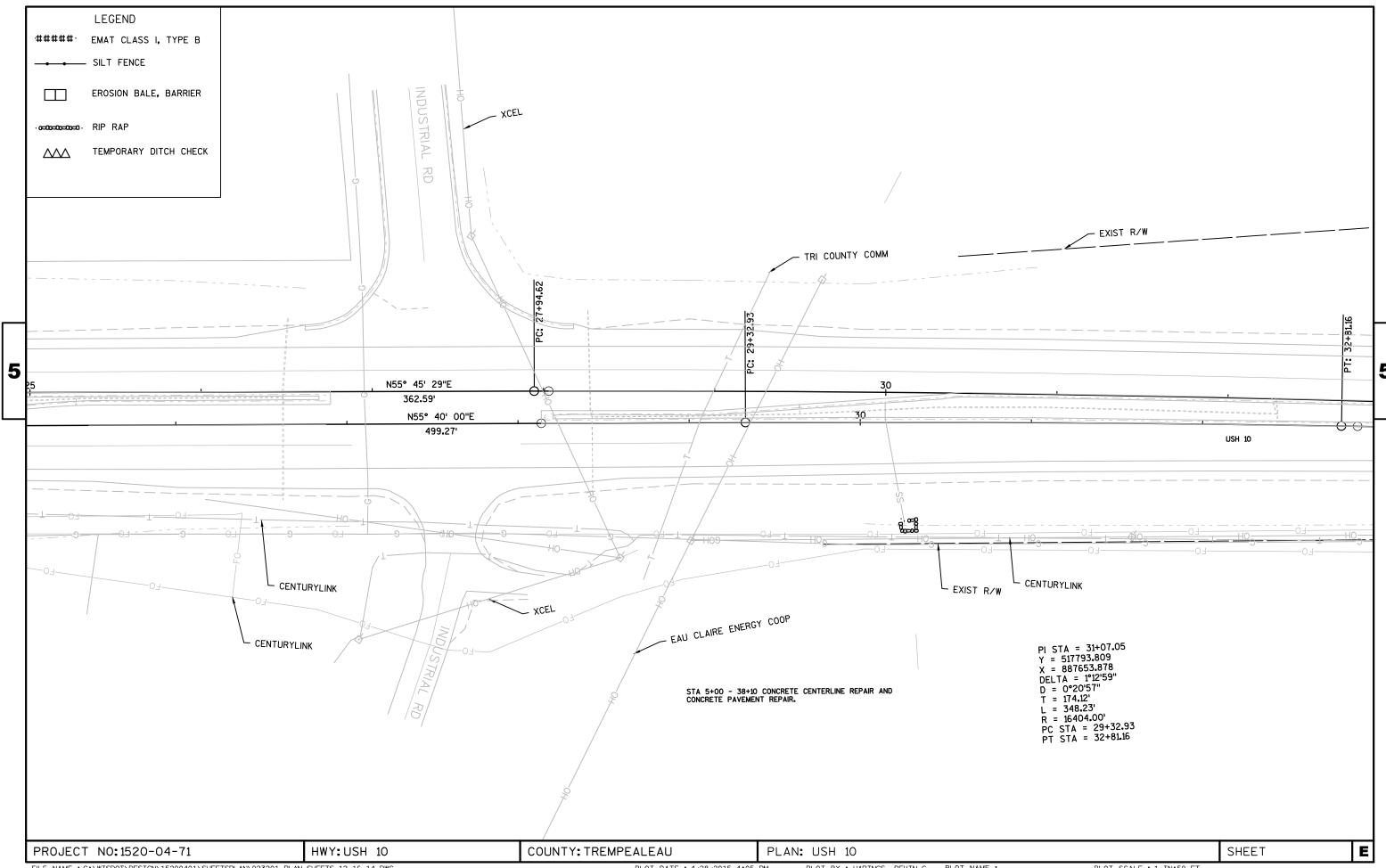
					SPV. 0195(01)			
CATEGORY	STATION TO	STATI ON	LOCATI ON		TON		REMARKS	
0010	38+03 -	87+97	USH 10		7753	MAI NLI NE,	SHLD, SIDE ROADS	
0010	87+97 -	118+20	USH 10		2540	MAI NLI NE,	SHLD, SI DE ROADS	
				TOTAL 0010	10293			

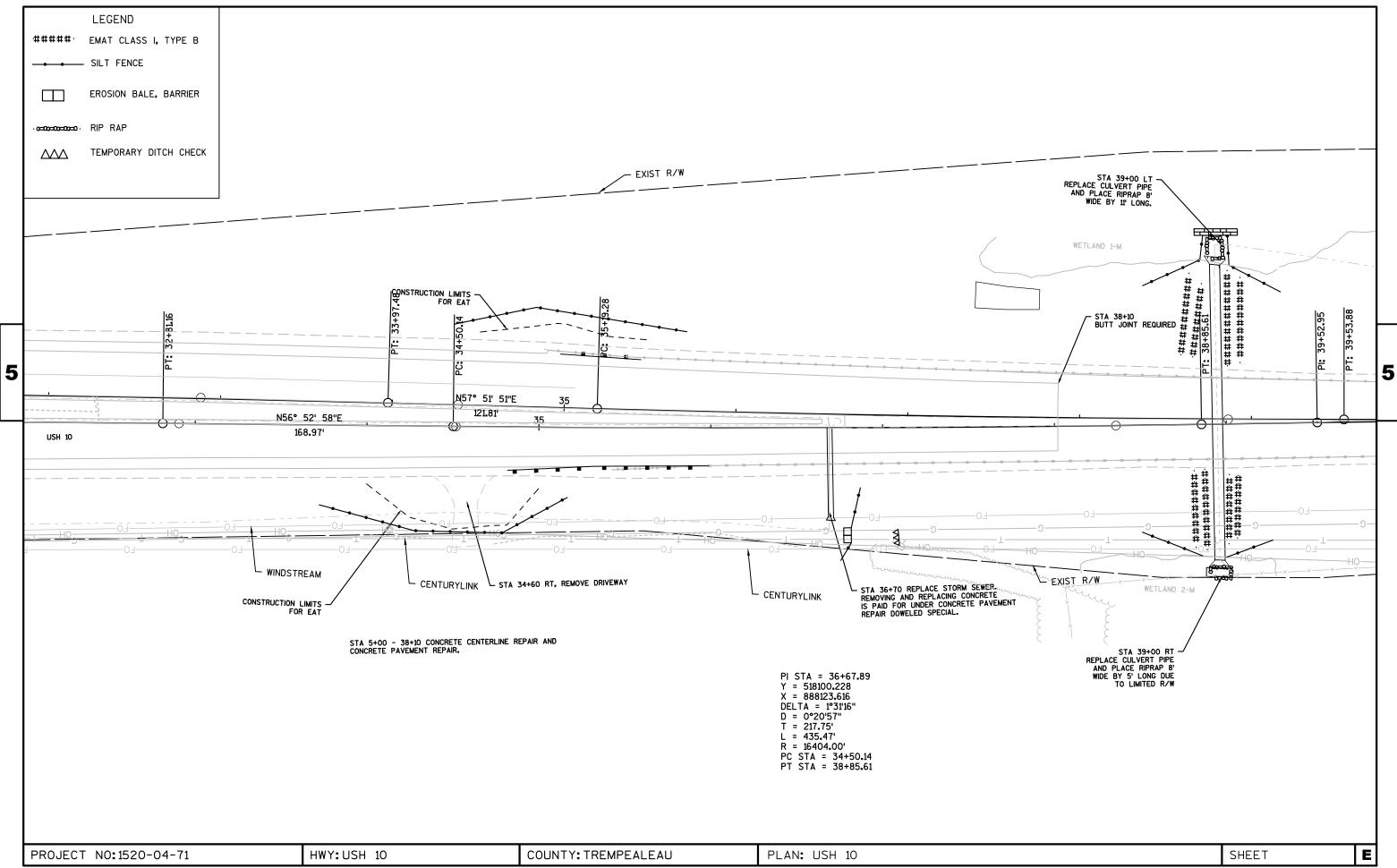
PROJECT NO: 1520-04-71 HWY: USH 10 COUNTY: TREMPEALEAU MISCELLANEOUS QUANTITIES SHEET: **E**

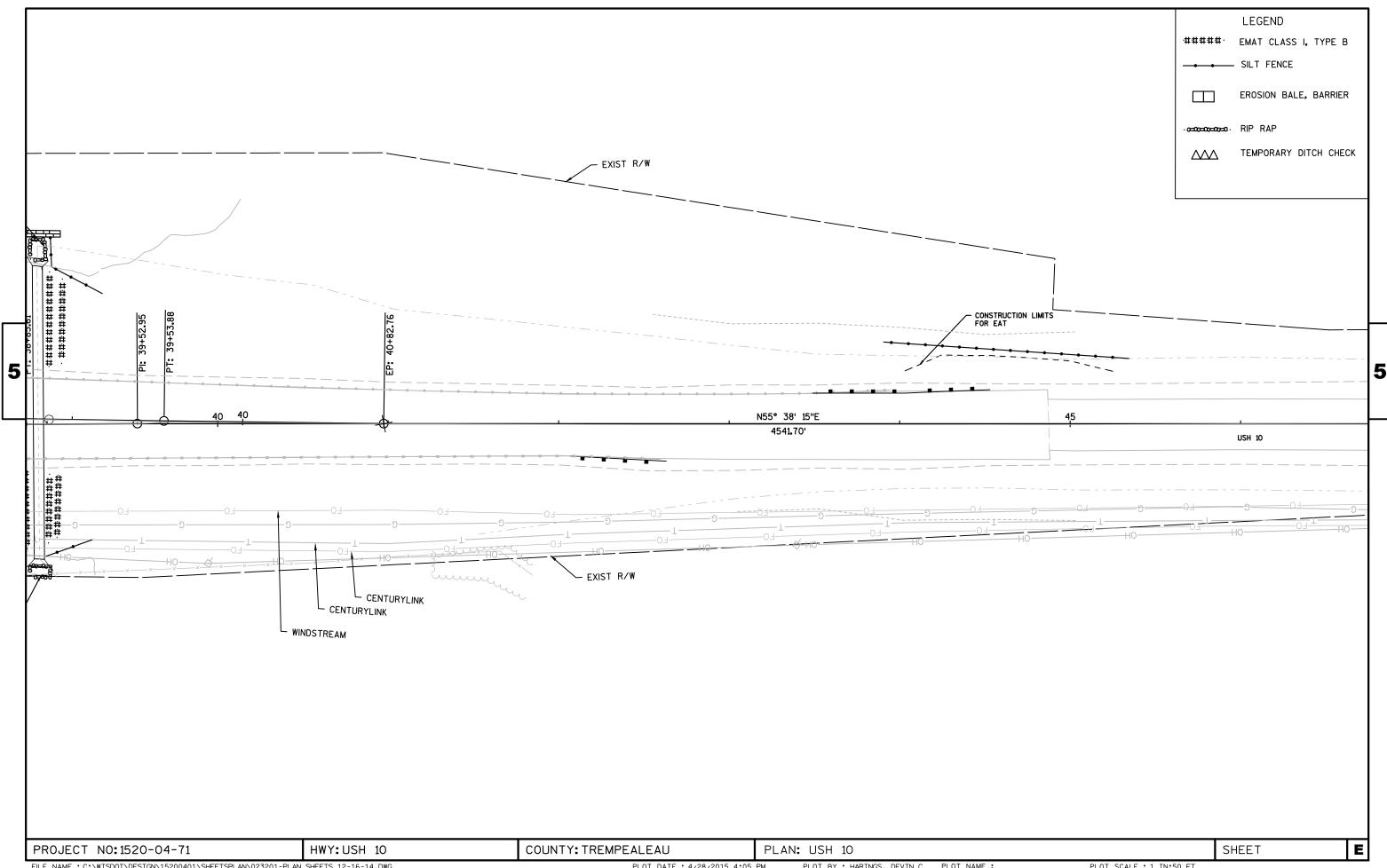


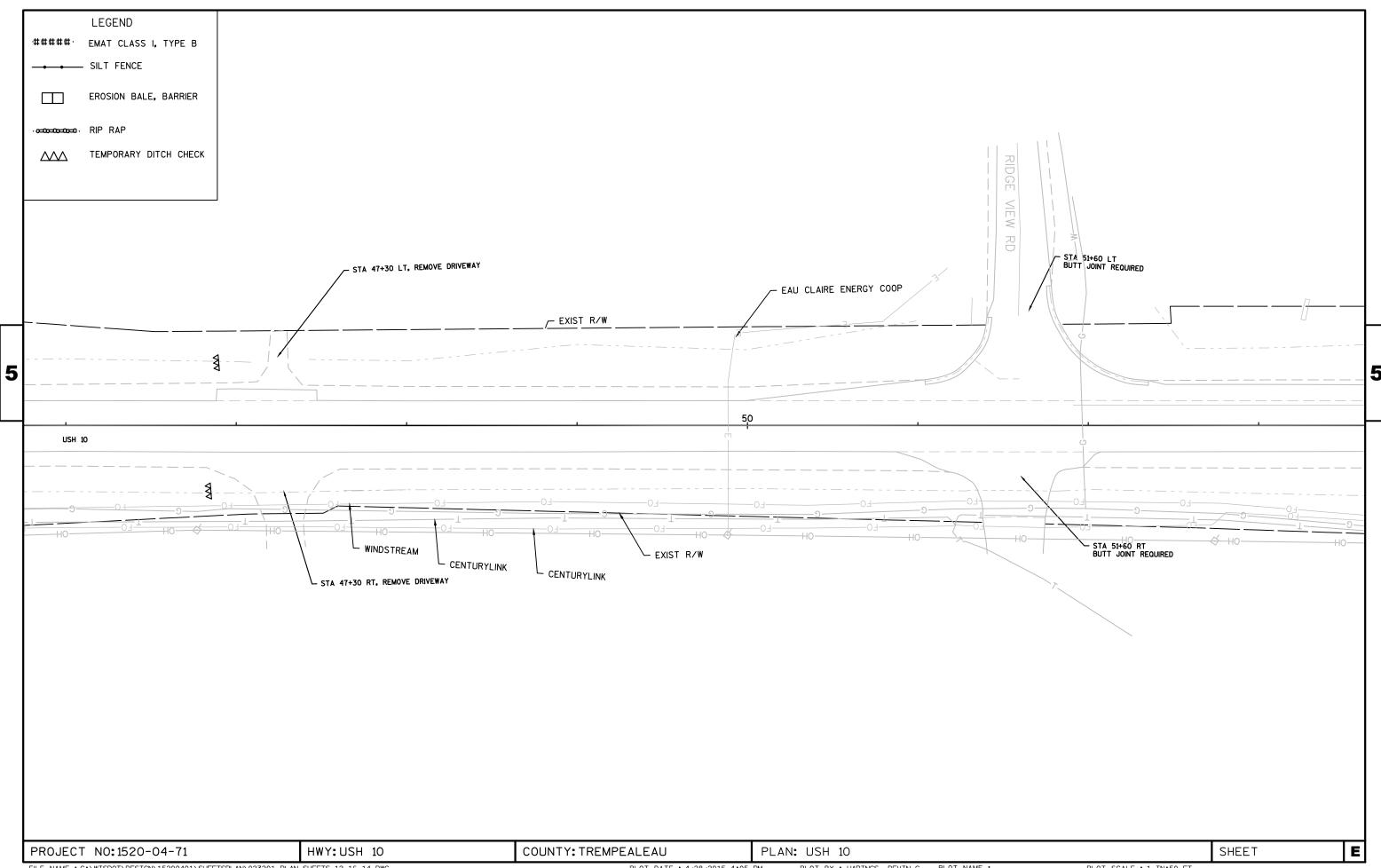


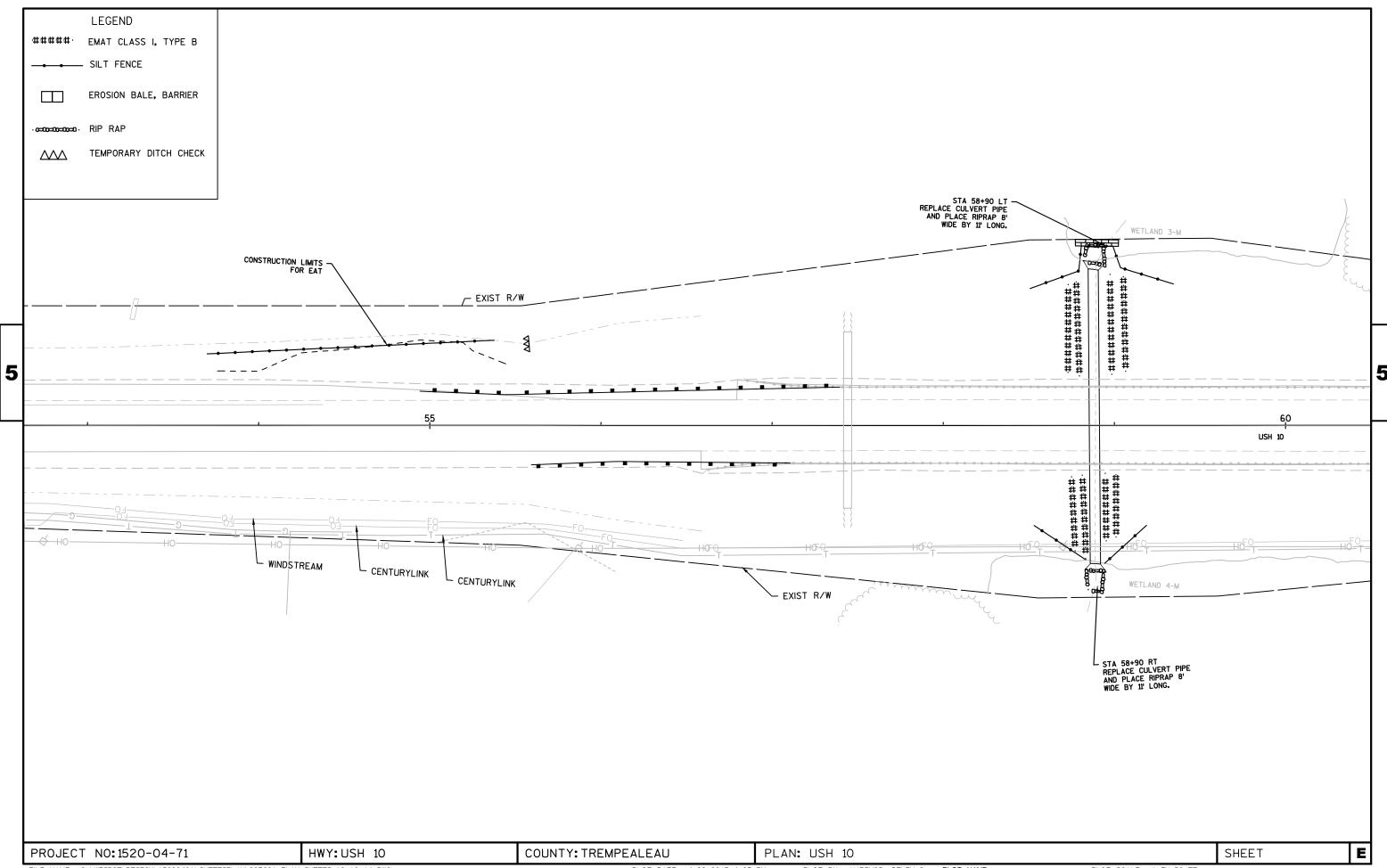


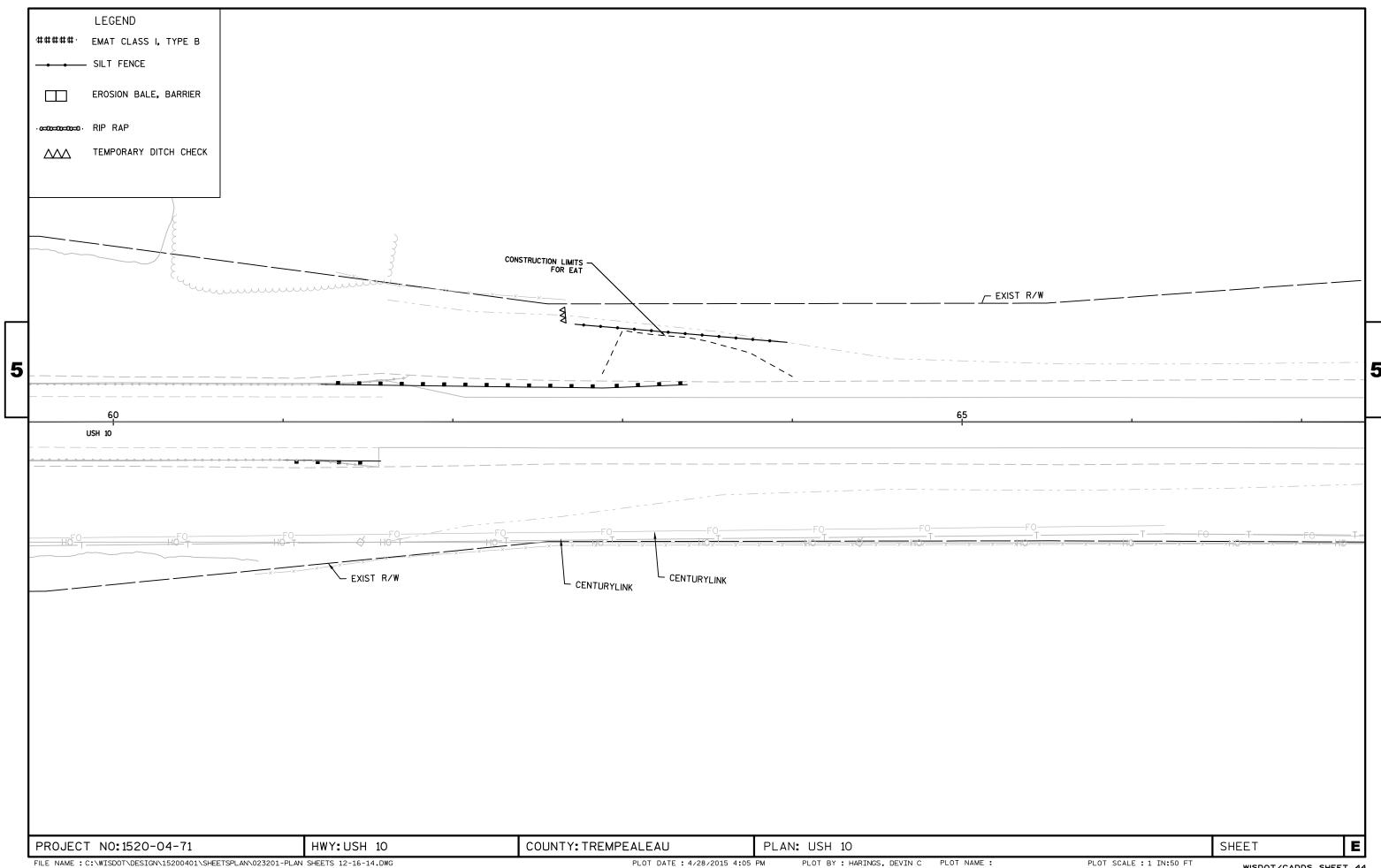


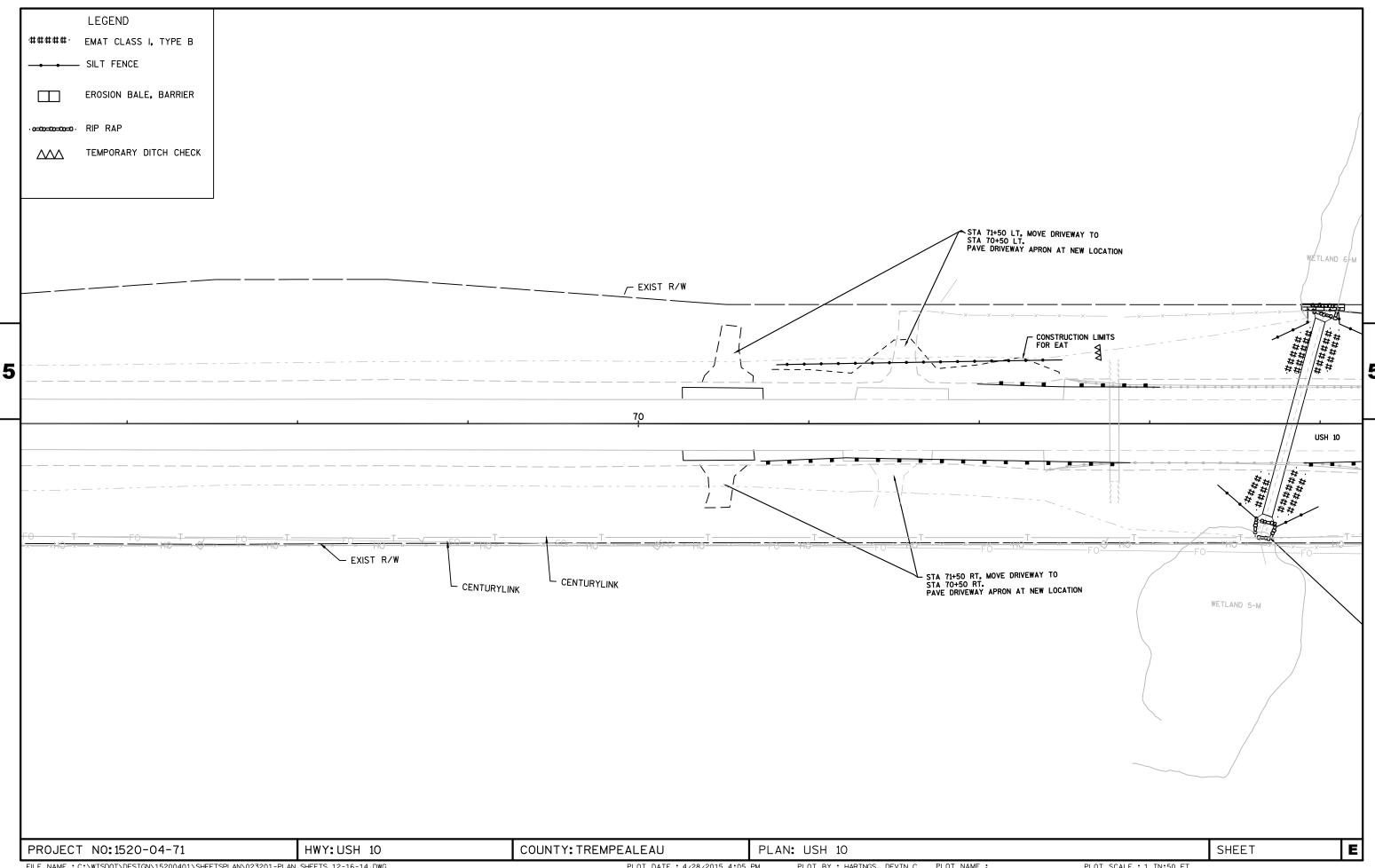


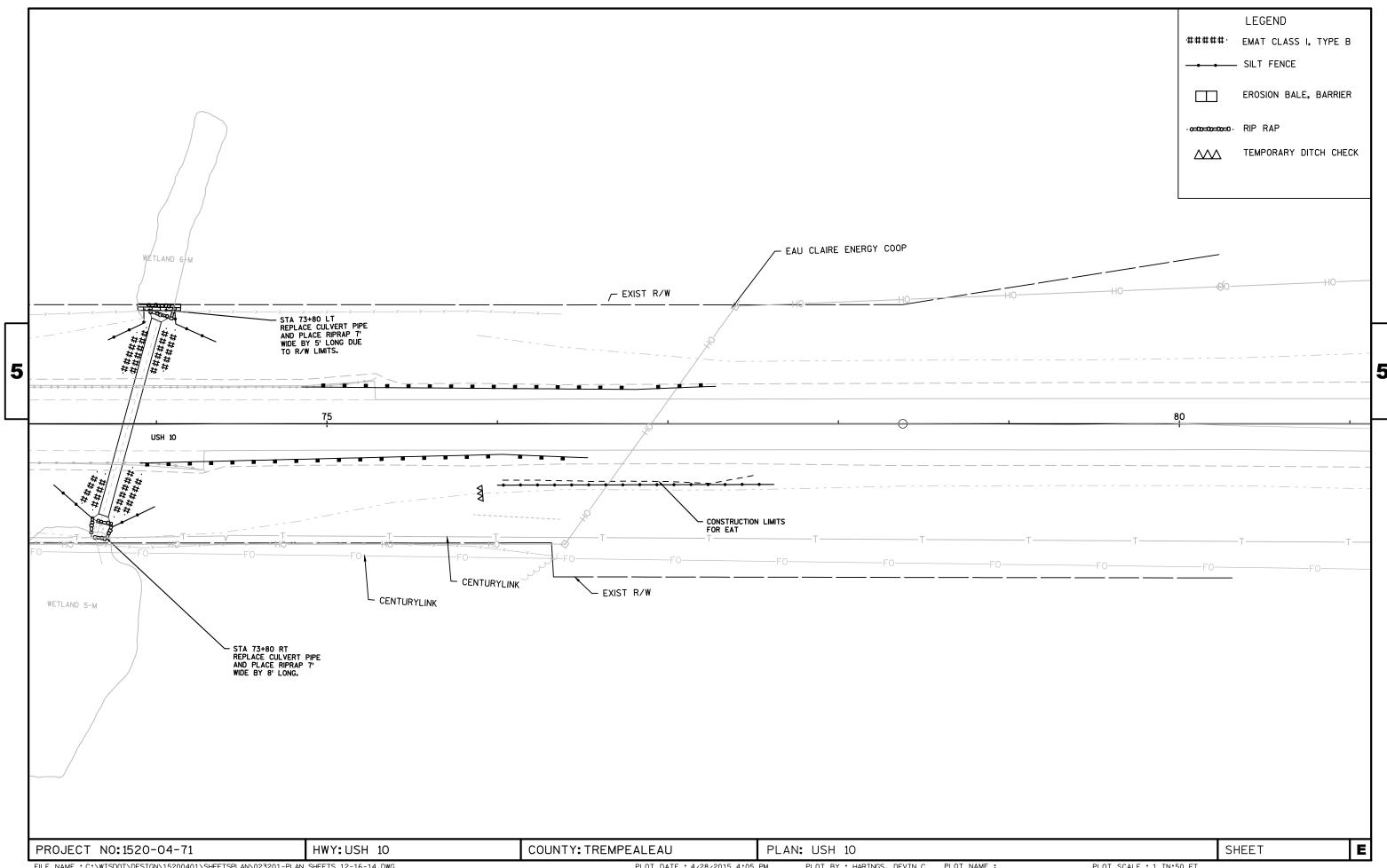


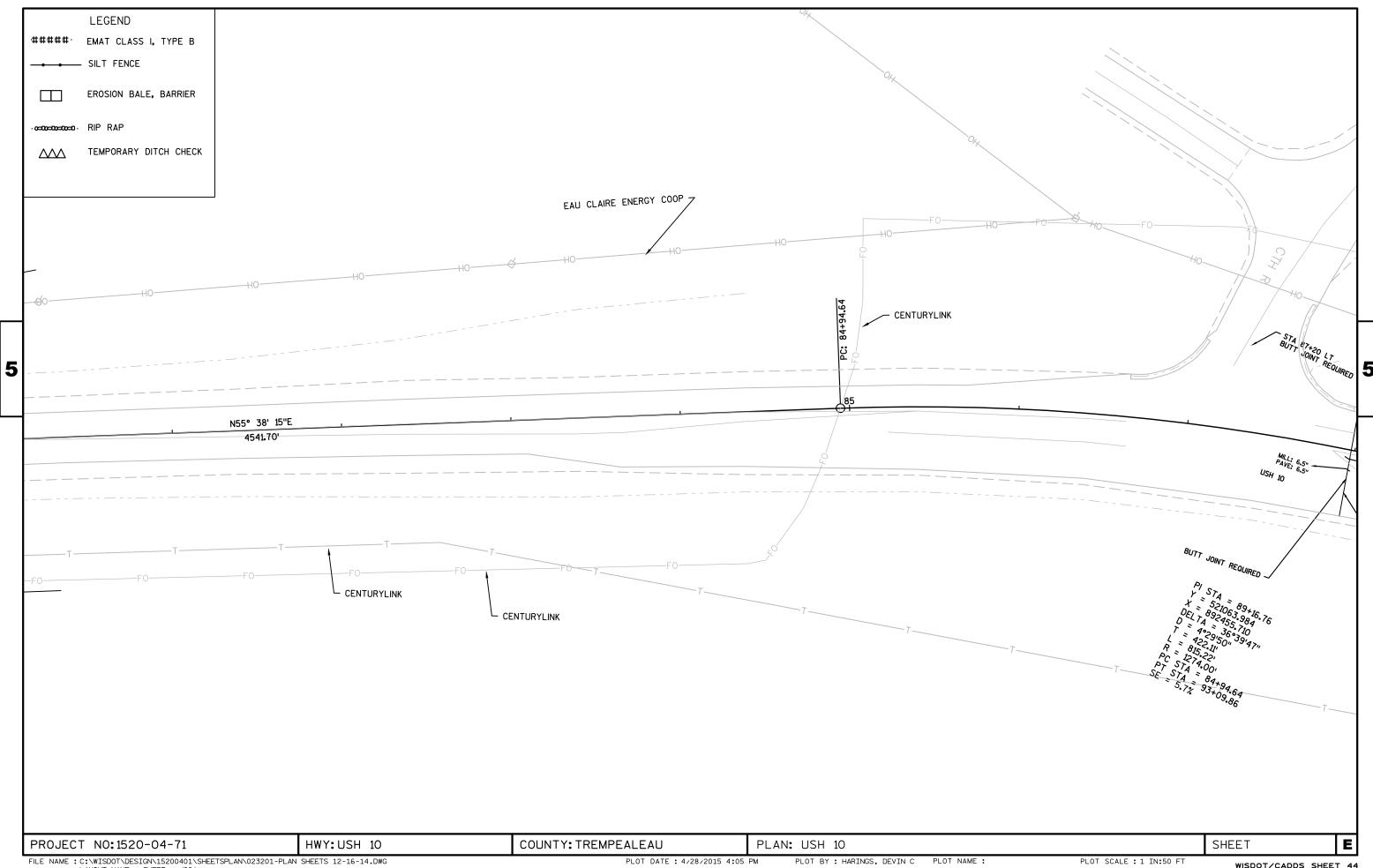


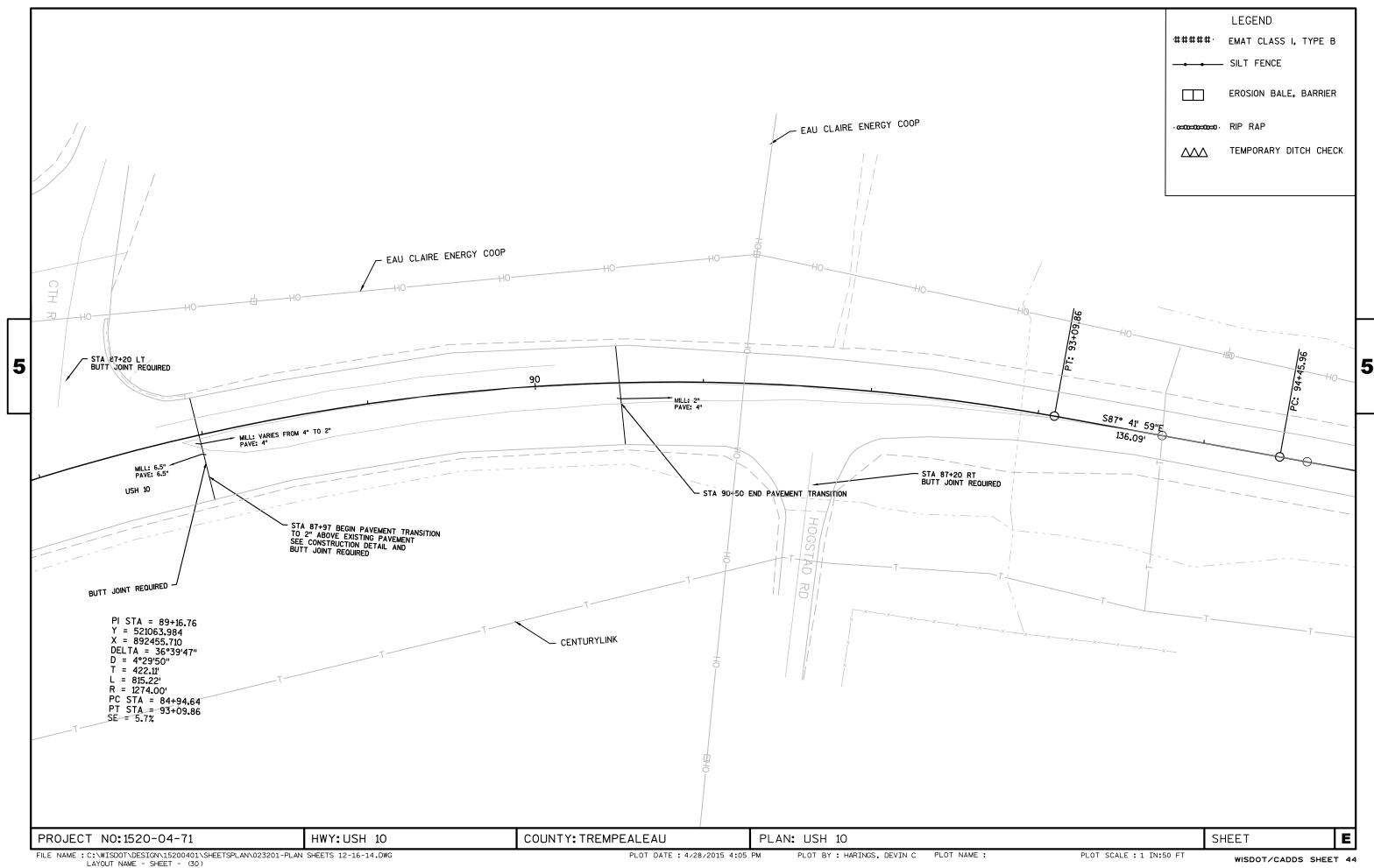


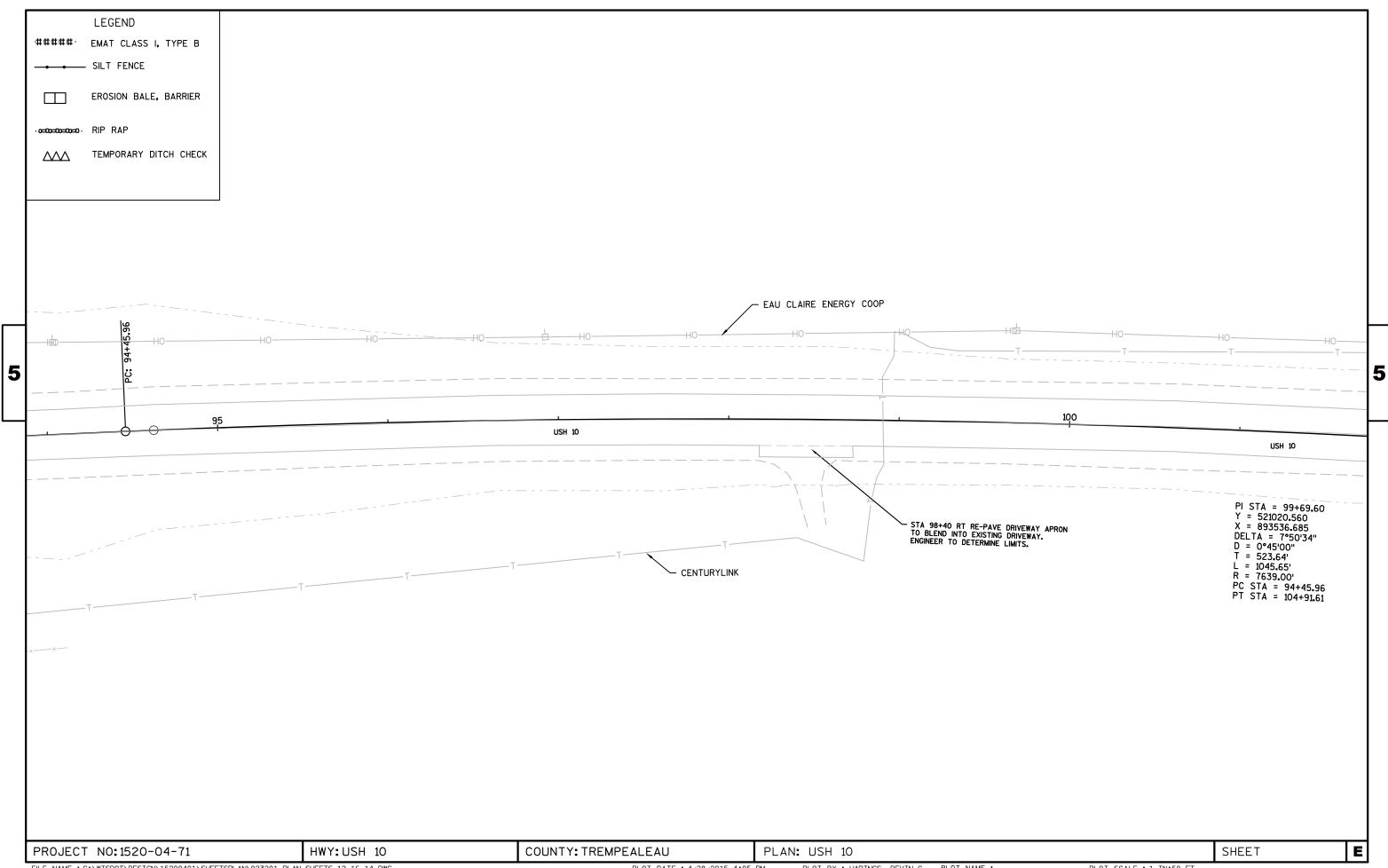


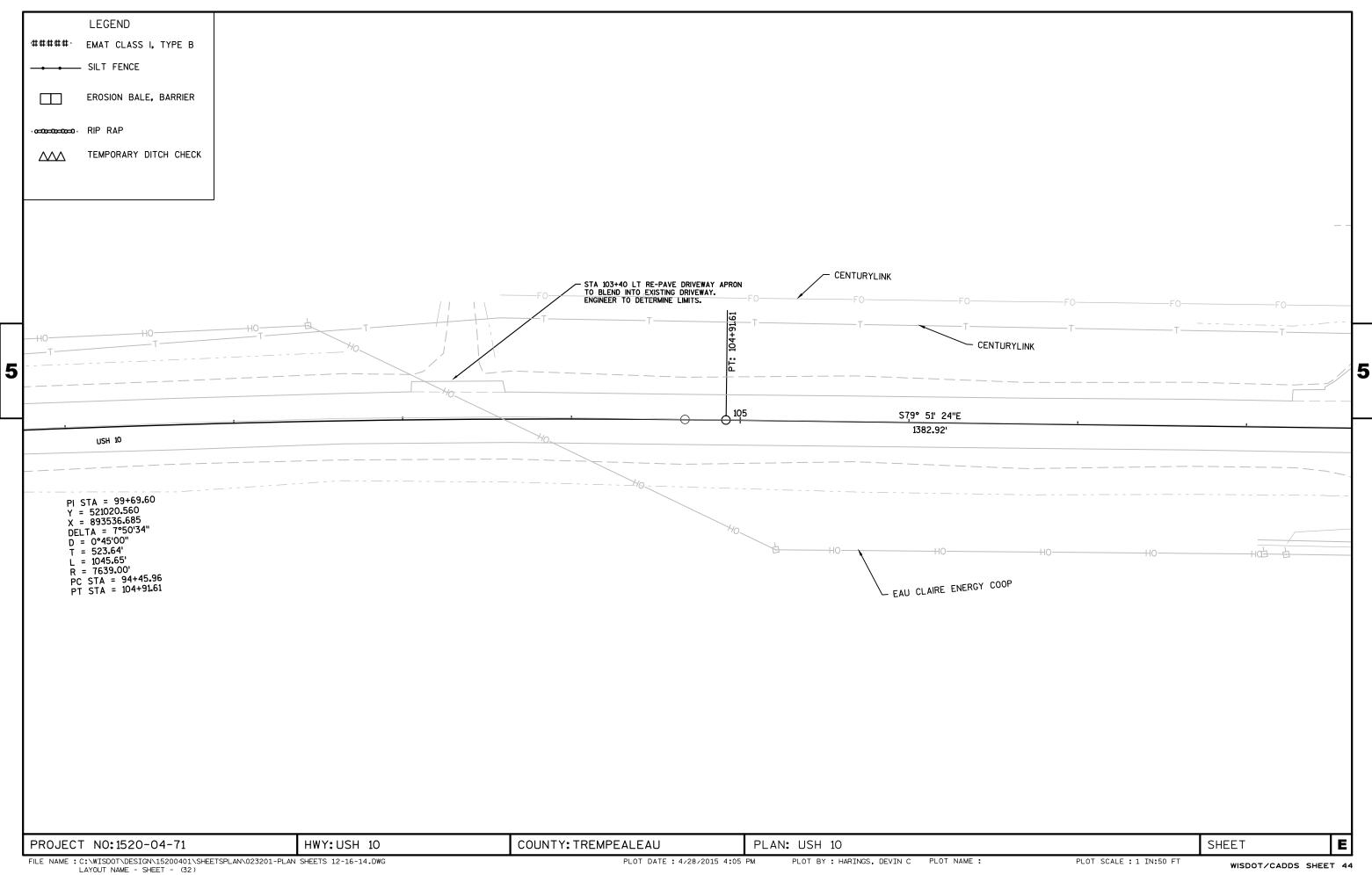


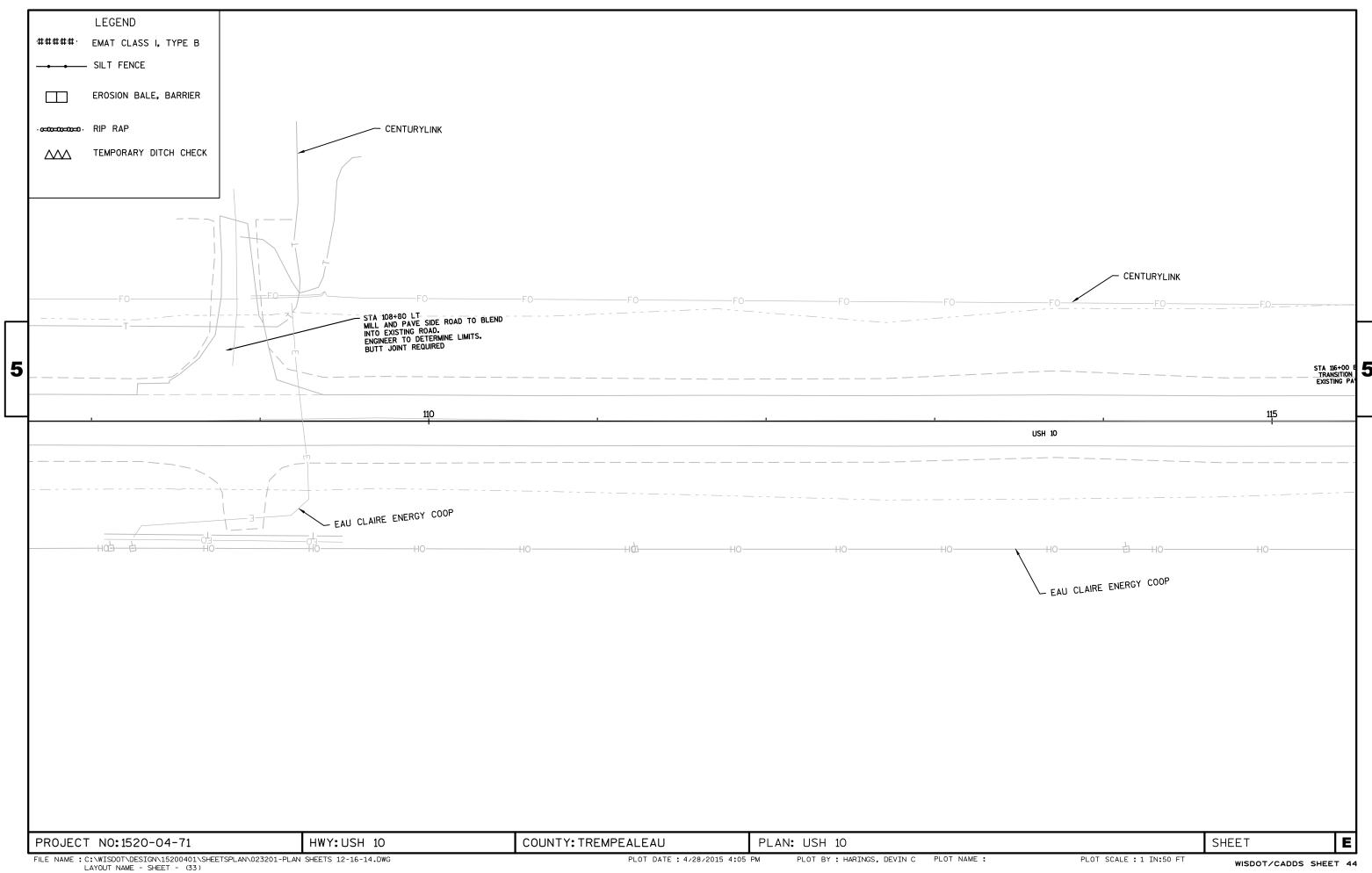


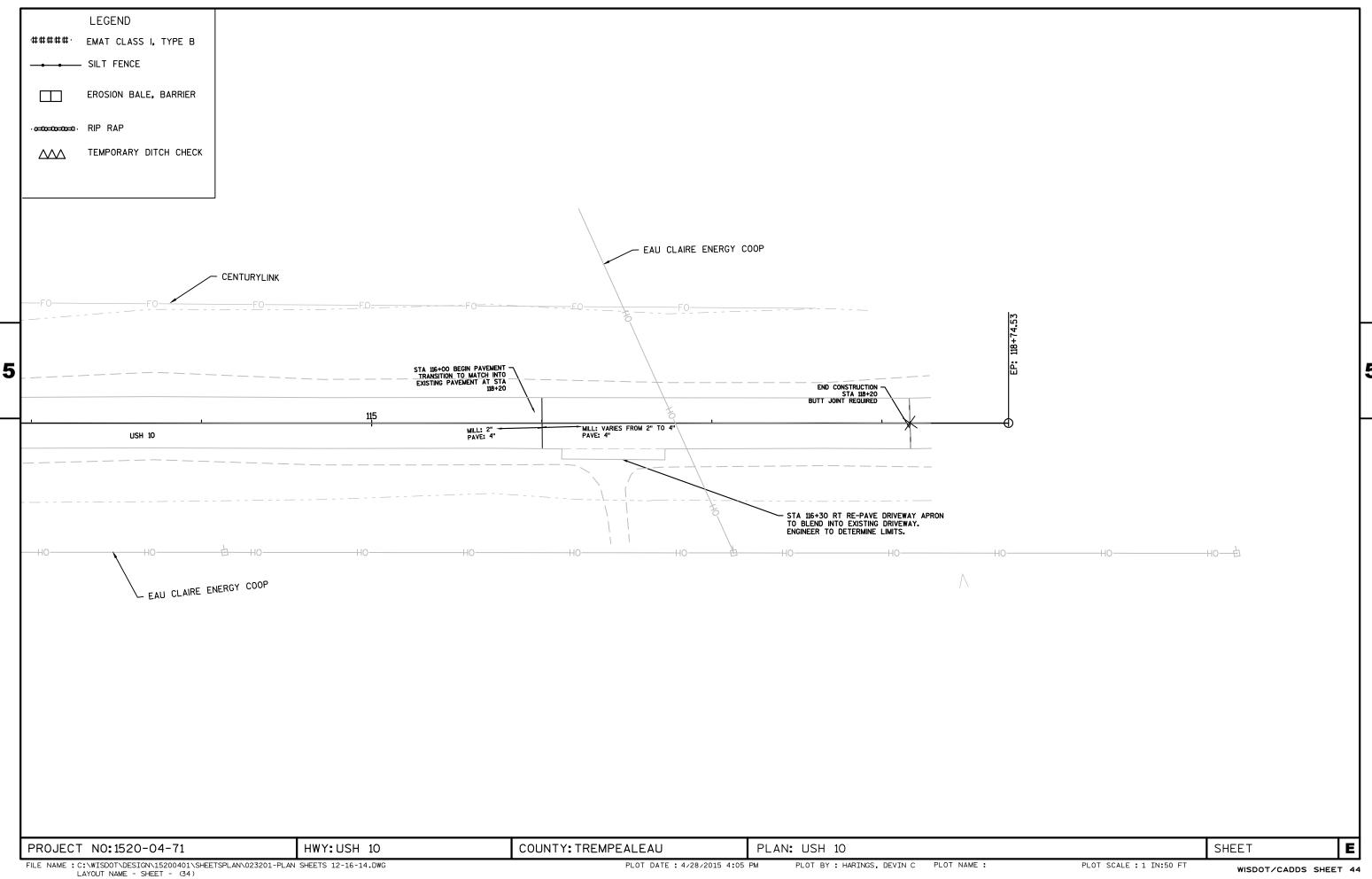












Standard Detail Drawing List

08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09A01-13A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
09A01-13B	AT-GRADE SIDE ROAD INTERSECTION, TYPE "A1" & "A2"
13C09-11A	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-11B	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
13C09-11C	CONCRETE PAVEMENT REPAIR AND REPLACEMENT
14B15-08A	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-08B	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-08C	STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS
14B18-06A	STEEL PLATE BEAM GUARD, CLASS "A" (AT BRIDGES, OBSTACLES AND SIDEROADS/DRIVEWAYS)
14B18-06B	STEEL PLATE BEAM GUARD, CLASS "A" AT MEDIAN APPROACH TO BRIDGES
14B24-08A	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B24-08B	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
14B24-08C	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-05A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-02	BARRI CADES AND SIGNS FOR SIDEROAD CLOSURES
15C07-12B	PAVEMENT MARKING WORDS
15C07-12C	PAVEMENT MARKING ARROWS
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C08-16C	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16D	PAVEMENT MARKING (CLIMBING LANE & PASSING LANE)
15C08-16E	PAVEMENT MARKING (LEFT TURN LANE)
15C18-03	MEDIAN ISLAND MARKING
15C21-05	SIGNING AND MARKING FOR TWO LANE TO FOUR LANE DIVIDED TRANSITIONS
15D15-01	TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE
15D20-03	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D21-03	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF **EROSION BALES / TEMPORARY** DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 /S/ Beth Connestro
CHIEF ROADWAY DEVELOPMENT ENGINEER

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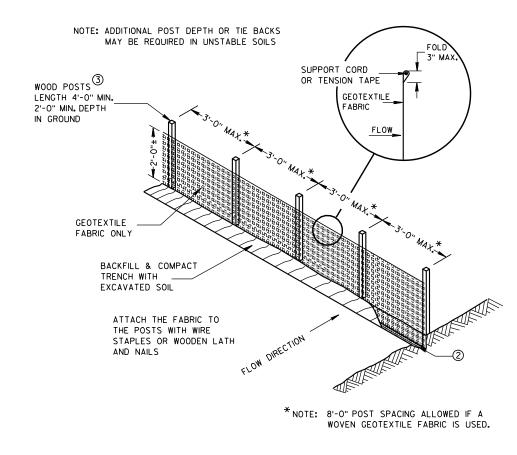
TYPICAL APPLICATION OF SILT FENCE

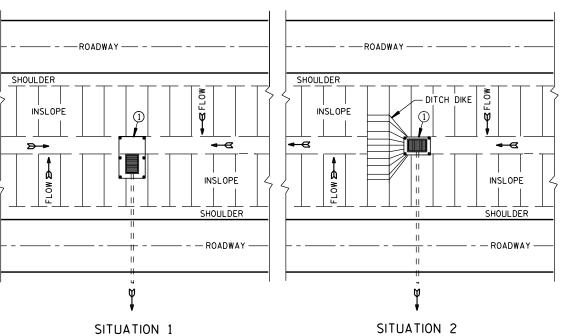
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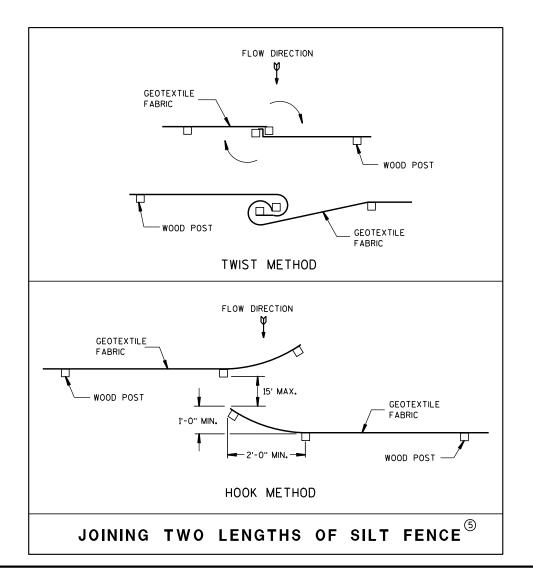
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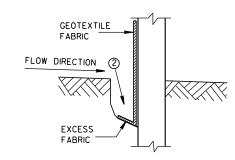
PLAN VIEW SILT FENCE AT MEDIAN SURFACE DRAINS



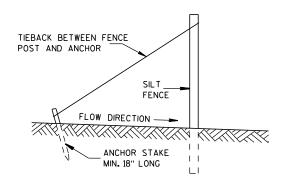
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- \bigcirc HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- 3 WOOD POSTS SHALL BE A MINIMUM SIZE OF 11/8" X 11/8" OF OAK OR HICKORY.
- 4) SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- (5) CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

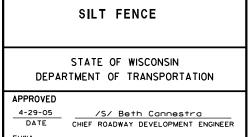


TRENCH DETAIL



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)



SILT FENCE

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	METAL APRON ENDWALLS										
PIPE	MIN. 1	MIN. THICK. DIMENSIONS (Inches)						APPROX.			
DIA.	(Incl		A	В	Н	L	Γį	L ₂	W	SLOPE	BODY
(IN.)	STEEL	ALUM.	(±1")	(MAX.)	(±1")	(±1 ½")	①	0	(±2")	320.2	
12	.064	.060	6	6	6	21	12	171/2	24	2½+o 1	1Pc.
15	.064	.060	7	8	6	26	14	213/4	30	2½to 1	1Pc.
18	.064	.060	8	10	6	31	15	281/4	36	21/2+o 1	1Pc.
21	.064	.060	9	12	6	36	18	295/8	42	21/2+o 1	1Pc.
24	.064	.075	10	13	6	41	18	371/4	48	21/2+o 1	1Pc.
30	.079	.075	12	16	8	51	18	521/4	60	21/2+0 1	1Pc.
36	.079	. 105	14	19	9	60	24	59¾	72	21/2+o 1	2 Pc.
42	.109	.105	16	22	11	69	24	75%	84	21/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 ¹ / ₄ +o 1	3 Pc.
54	.109	.105	18	30	12	84	30	851/2	102	2 ¹ / ₄ †o 1	3 Pc.
60	.109×	.105×	18	33	12	87	_	_	114	2 to 1	3 Pc.
66	.109×	.105×	18	36	12	87	_	_	120	2 to 1	3 Pc.
72	.109×	.105×	18	39	12	87	_	_	126	2 to 1	3 Pc.
78	.109×	.105×	18	42	12	87	_	_	132	11/2+0 1	3 Pc.
84	.109×	.105×	18	45	12	87	_	_	138	11/2 to 1	3 Pc.
90	.109×	.105×	18	37	12	87	_	_	144	11/2+0 1	3 Pc.
96	.109×	.105×	18	35	12	87	_	_	150	1/2+0 1	3 Pc.

	REINFORCED CONCRETE APRON ENDWALLS							
PIPE			DIM	ENSIONS	(Inches)			APPROX.
DIA.	T	A	В	С	D	Ε	G	SLOPE
12	2	4	24	48 1/8	721/8	24	2	3 to 1
15	21/4	6	27	46	73	30	21/4	3 to 1
18	21/2	9	27	46	73	36	21/2	3 to 1
21	23/4	9	36	371/2	731/2	42	23/4	3 to 1
24	3	91/2	431/2	30	731/2	48	3	3 to 1
27	31/4	101/2	491/2	24	731/2	54	31/4	3 to 1
30	$3\frac{1}{2}$	12	54	193/4	731/2	60	31/2	3 to 1
36	4	15	63	34¾	97¾	72	4	3 to 1
42	$4\frac{1}{2}$	21	63	35	98	78	41/2	3 to 1
48	5	24	72	26	98	84	5	3 to 1
54	51/2		65	**************************************	8 ¹ / ₄ - 100	90	51/2	2% to 1
60	6	* ** 30-35	60	39	99	96	5	2 to 1
66	61/2	* ** 24-30	* * * 72-78	* * * 21-27	99	102	51/2	2 to 1
72	7	* ** 24-36	78	21	99	108	6	2 to 1
78	71/2	* ** 24-36	78	21	99	114	61/2	2 to 1
84	8	36	901/2	21	1111/2	120	61/2	1½+o 1
90	81/2	41	871/2	24	1111/2	132	61/2	11/2+0 1

THREADED %6" DIA. ROD CONNECTOR AROUND CULVERT & THROUGH TANK TYPE CONNECTOR LUG LUG OR ALTERNATE CONNECTOR STRAP (SEE DETAIL) MEASURED LENGTH OF CULVERT TYPE 1 FOR 12" THRU 24" CORR. PIPE







NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL. AND CORRUGATED BAND FITS INSIDE ENDWALL.

CORRUGATED PIPE. FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5

DIMPLED BAND MAY BE USED WITH HELICALLY

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT ALTERNATE FOR TYPE 1 CONNECTION END SECTION CONNECTOR STRAP

* EXCEPT CENTER PANEL SEE GENERAL NOTES





SHOULDER

SLOPE



SIDE ELEVATION METAL ENDWALLS



**MAXIMUM





CONCRETE ENDWALLS

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA, GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE

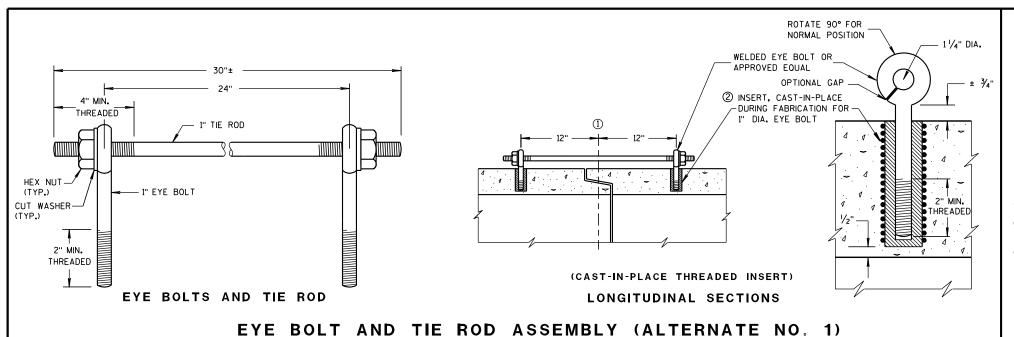
LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES. THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

(1) FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.



11/30/94 /S/ Rory L. Rhinesmith CHIEF ROADWAY DEVELOPMENT ENGINEER



GENERAL NOTES

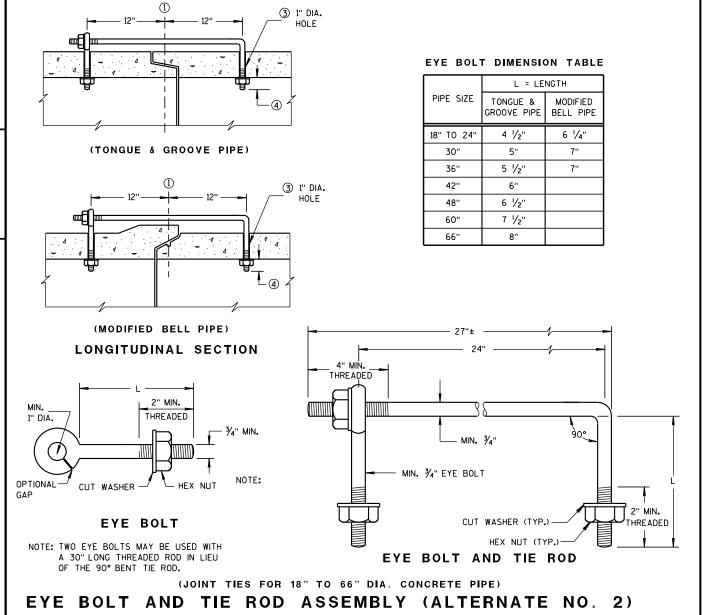
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES, ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

- (1) & OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE
- ${\mathfrak S}$ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ${\mathfrak L}$ OF TONGUE AND GROOVE.
- 4 BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- (5) OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN $rac{1}{2}$ INCH OF THE INNER SURFACE OF THE PIPE.

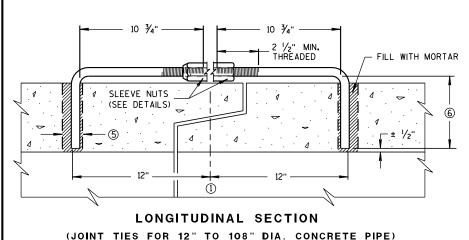


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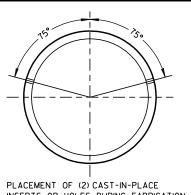
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ADJUSTABLE TIE ROD TABLE 5/8 5 12-60 3/4 5 1/2 3/4 90-108 DIMENSIONS SHOWN ARE IN INCHES **TAPERED** PLAIN RIGHT AND LEFT THREADS **SLEEVE NUTS**

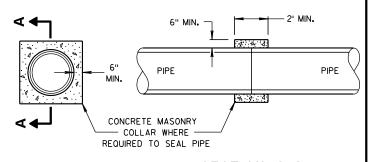


ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A-A

CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

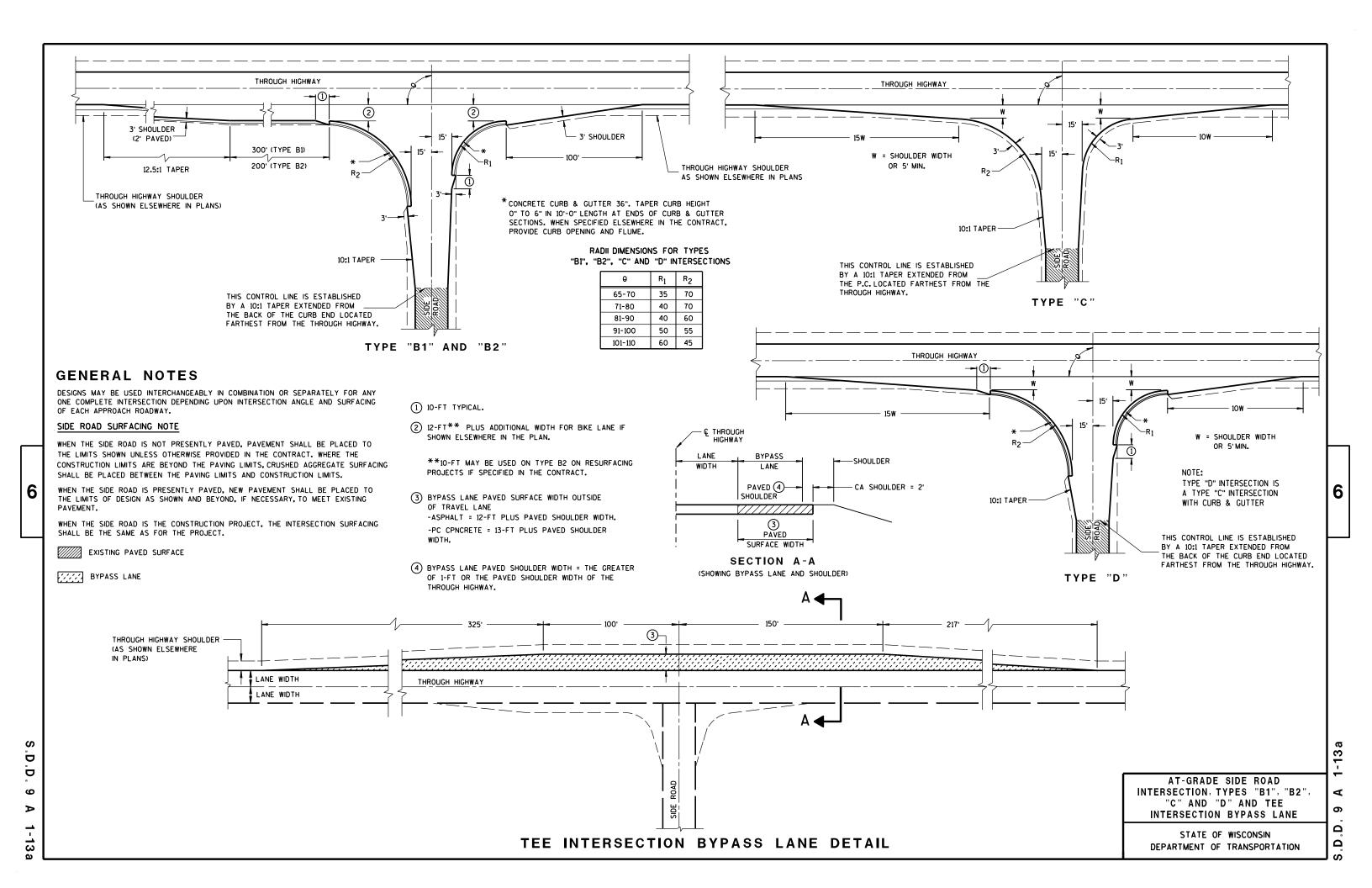
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

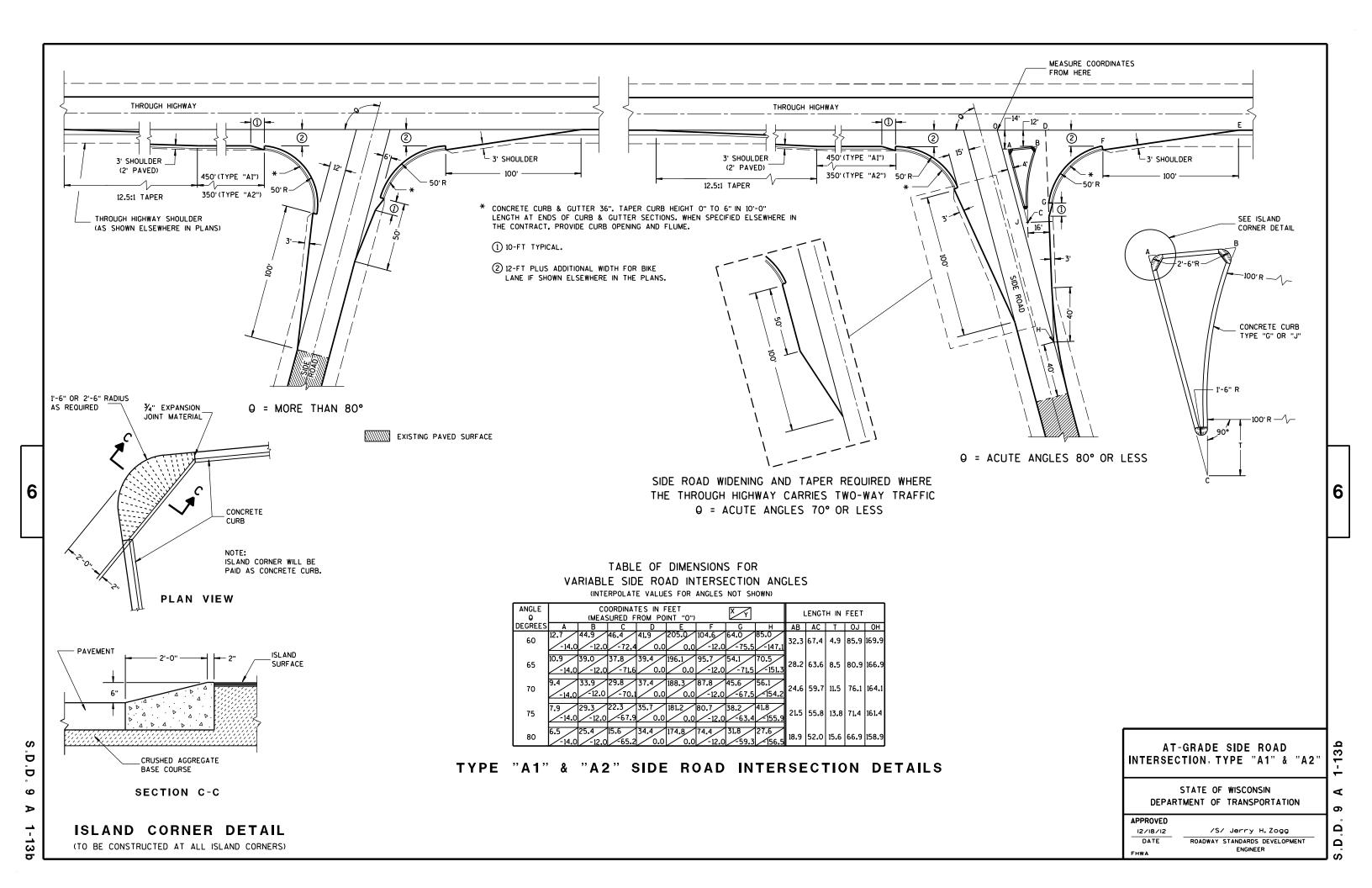
6/5/2012

/S/ Jerry H. Zogg DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

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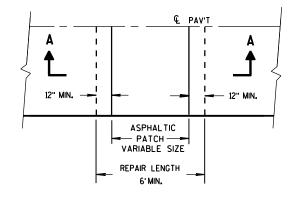




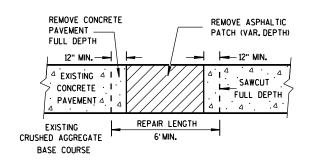
PROVIDE A 6-FOOT MINIMUM DISTANCE FROM BOUNDARIES OF CONCRETE REPAIR AREAS TO ADJACENT TRANSVERSE JOINT OR CRACK IN THE SAME LANE.

THE LENGTH OF THE REPAIRS MAY VARY FROM THE DIMENSIONS SHOWN IF THE EXISTING CONCRETE PAVEMENT IS NONDOWELED AND THE PAVEMENT IS TO BE OVERLAID AFTER REPAIRING.

1) DOWEL BARS MIGHT NOT EXIST.

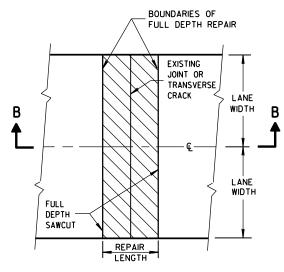


PLAN VIEW

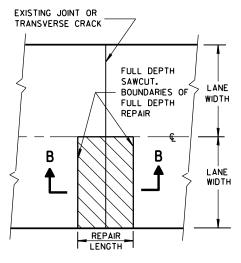


SECTION A-A

HMA PATCH REMOVAL



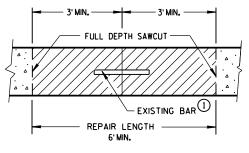
PLAN VIEW (DOUBLE LANE REPAIR)



PLAN VIEW (SINGLE LANE REPAIR)

FULL DEPTH CONCRETE PAVEMENT REMOVAL

(SEE NOTE)



SECTION B-B
CONCRETE REMOVAL

CONCRETE PAVEMENT REPAIR
AND REPLACEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

MAXIMUM TIE BAR PAVEMENT CLEAR COVER SPACING "S" DEPTH PAVEMENT WIDTH "D" 24' OR 26' ≥30' 42" 3"±1/2" 48" 6,6 1/2" 3 1/4"±1" 36" 7, 7 1/2" 3 ¾"±1" 39" 30" 8, 8 1/2" 9,9 1/2" 4 1/4"±1" 33" 27" 10, 10 1/2" 4 3/4"±1" 30" 24" 11, 11 1/2" 5 1/4"±1" 27" 21" 12" 5 ¾"±1" 21" 24"

1/4" RAD.

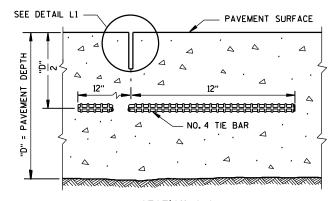
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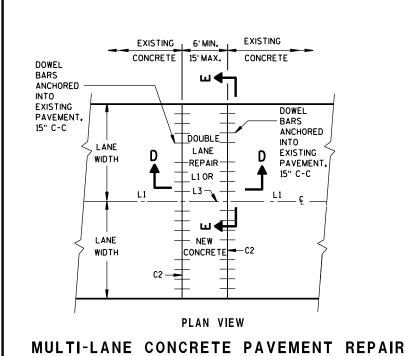
TIE BAR TABLE

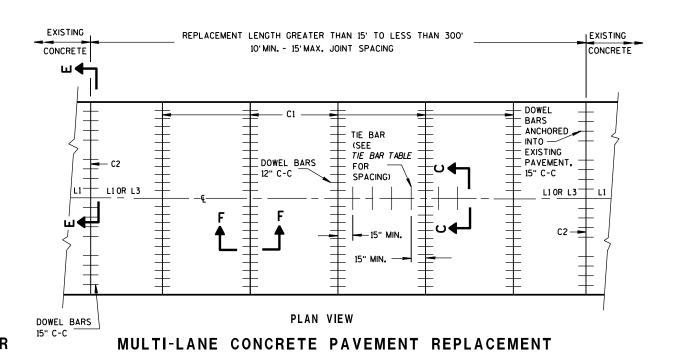


SECTION C-C SAWED LONGITUDINAL JOINT

SEE DETAIL C1 DOWEL BARS @ 12" C-C 12" FROM PAVEMENT EDGE (SEE SIZE TABLE)

SECTION F-F **CONTRACTION JOINT**





GENERAL NOTES

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

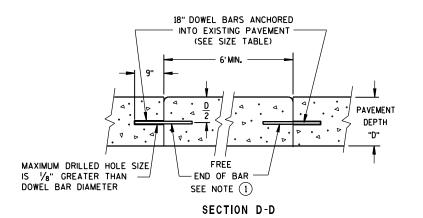
CONCRETE PAVEMENT REPAIRS OF EXISTING NONDOWELED CONCRETE PAVEMENTS DO NOT NEED TO BE DOWELED.

DO NOT SEAL OR FILL JOINTS.

ANCHOR DOWEL BARS AND TIE BARS INTO DRILLED HOLES WITH AN EPOXY.

FOR MULTI-LANE CONCRETE PAVEMENT REPLACEMENTS, PROVIDE A MINIMUM DISTANCE OF 15 INCHES FROM ALL TRANSVERSE JOINTS OR EDGES OF REPLACEMENT TO THE CENTER OF THE TIE BAR NEAREST THAT JOINT

(1) APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.



(FOR 11'LANE WIDTH REDUCE CENTER SPACE TO 1'-O") 1'-3",1'-3" | 1'-3",1'-3",1'-3", 2'-0",1'-3",1'-3",1'-3" **PAVEMENT** DEPTH 0.0.0 "D" 18" DOWEL BARS (SEE SIZE TABLE)

DRILLED DOWEL BAR CONSTRUCTION JOINT

SECTION E-E

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

AILD COIN	. OI AGIN	G INDEL
PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 1/2", 6",6 1/2"	NONE	12'
7",7 1/2"	1"	14'
8",8 1/2"	1 1/4"	15'
9",9 1/2"	1 1/4"	15'
10" & ABOVE	1 1/2"	15'

CONCRETE PAVEMENT REPAIR AND REPLACEMENT

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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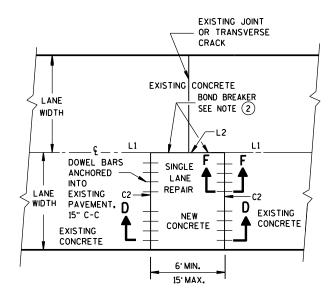
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SECTION G-G

TIE BARS ANCHORED INTO EXISTING PAVEMENT



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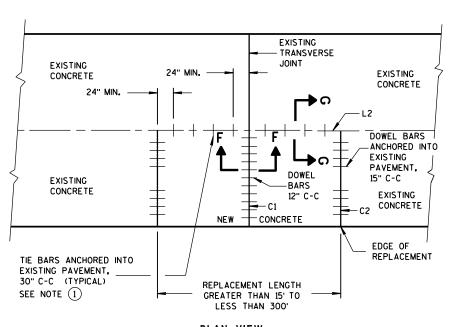
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PLAN VIEW
SINGLE LANE
CONCRETE PAVEMENT REPAIR



PLAN VIEW
SINGLE LANE
CONCRETE PAVEMENT REPLACEMENT

GENERAL NOTES

- (1) WITH THE APPROVAL OF THE ENGINEER, FOR SINGLE LANE PAVEMENT REPLACEMENTS LESS THAN 30 FEET IN LENGTH, THE CONTRACTOR MAY INSTALL DRILLED TIE BARS ON 6:1 SKEW HORIZONTALLY, DIRECTION OF SKEW ALTERNATING WITH EACH SUCCESSIVE BAR. DRIVE SKEWED TIE BARS TO A DEPTH OF 6 INCHES AND TO SUCH A DIAMETER AS TO PROVIDE A TIGHT DRIVEN FIT.
- 2 USE AN ENGINEER-APPROVED BOND BREAKER (E.G. RELEASE AGENT, CURING COMPOUND) FOR SINGLE LANE REPAIRS UP TO 15 FEET IN LENGTH.

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CONCRETE PAVEMENT REPAIR AND REPLACEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

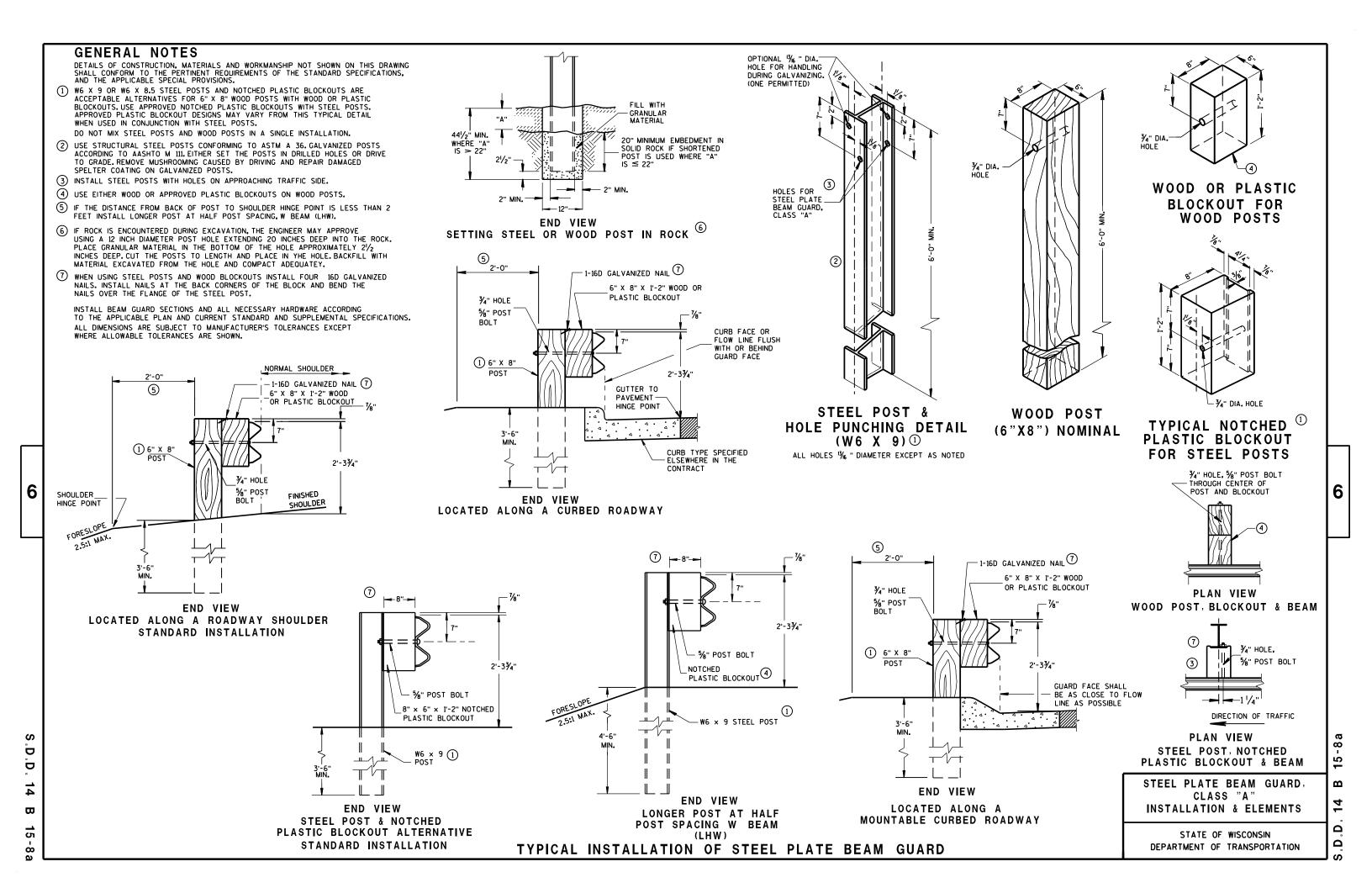
12-2013
DATE

APPROVED

/S/ Deb Bischoff
PAVEMENT POLICY & DESIGN ENGINEER

FHWA

S/ Deb Bischoff T POLICY & DESIGN ENGINEER



FRONT VIEW

POST SPACING STANDARD INSTALLATION

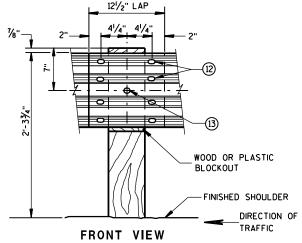
SECTION THRU W

SYMMETRICAL

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BEAM

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BEAM SPLICE AT WOOD POST AND POST MOUNTING DETAIL

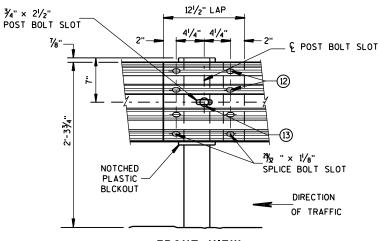
GENERAL NOTES

- (8) PROVIDE SILVER REFLECTIVE SHEETING ON ALL REFLECTORS EXCEPT THOSE LOCATED ALONG THE LEFT EDGE OF ONE-WAY ROADWAYS, WHICH SHALL BE PROVIDED WITH YELLOW REFLECTIVE SHEETING. SHEETING IS TYPE H. SEE STANDARD SPECIFICATION 637.
- (9) DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.
- (10) REVERSE EVERY OTHER REFLECTOR FOR 2-WAY VISIBILITY. THE CONTRACTOR MAY FURNISH TWO-SIDED REFLECTORS IN LIEU OF ONE-SIDED REFLECTORS.
- (11) PROVIDE AN ANGLE OF BEND OF 90° ± 1° FOR TWO-SIDED REFLECTORS.
- (12) 8 5%" * X 2" BUTTON HEAD BOLTS WITH OVAL SHOULDERS & RECESS NUTS.
- (3) %" DIA. BUTTON HEAD BOLT AND RECESS NUT WITH %" DIA. F844 FLAT WASHER UNDER NUT.

12'-6" OR 25'-0" EFFECTIVE LENGTH OF BEAM 3'-1\/2" C-C 3'-1\/2" C-C 3'-1\/2" C-C 3'-1\/2" C-C POST POST POST POST SPACING SPACING SPACING SPACING FINISHED DIRECTION OF TRAFFIC

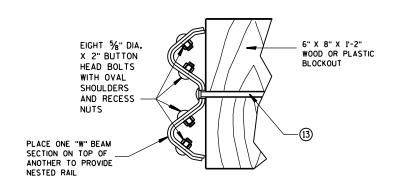
FRONT VIEW

POST SPACING FOR LONGER POST AT HALF POST SPACING W BEAM (LHW)



FRONT VIEW
BEAM SPLICE AT STEEL POST

TYPICAL SPLICING DETAILS OF STEEL PLATE BEAM GUARD

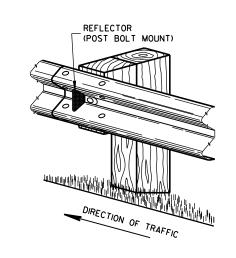


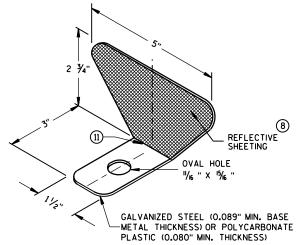
NESTED W BEAM (NW)

USE ALL OTHER STANDARD BEAM GUARD DETAILS FOR CONSTRUCTING NESTED W BEAM (NW)

	9
REFLECTOR	SPACING

	BEAM GUARD	REFLECTOR	NO. SURFACES	MIN. NO.
	LENGTH	SPACING	REFLECTORIZED	REFLECTORS
ONE WAY TRAFFIC	< 500,	50' C-C	1 1	3
TWO WAY TRAFFIC	> 500. < 500.	25' C-C 50' C-C	1 100	6
TWO WAY TRAFFIC	> 500, < 500,	50' C-C 100' C-C	2(11)	3





ONE SIDED REFLECTOR DETAIL AND TYPICAL INSTALLATION $^{\circ}$

STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

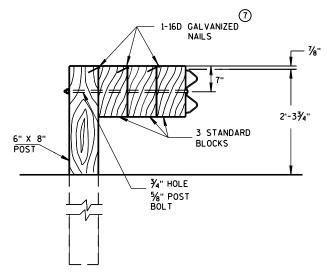
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DETAIL FOR DOUBLE BLOCKS

THE NUMBER OF DOUBLE BLOCK POSTS WITHIN A BARRIER RUN IS UNLIMITED

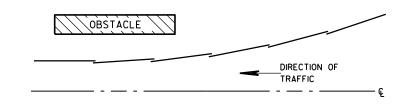


DETAIL FOR TRIPLE BLOCKS

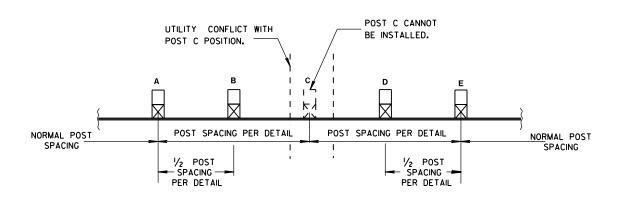
TRIPLE BLOCK DETAIL IS LIMITED TO ONE LOCATION WITHIN A BEAM GUARD RUN.

NOTES: USE DOUBLE OR TRIPLE BLOCKS WHEN UNDERGROUND OBSTACLES PREVENT THE POST FROM BEING INSTALLED.

DO NOT USE EXTRA BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.



PLAN VIEW BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION

STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS 6

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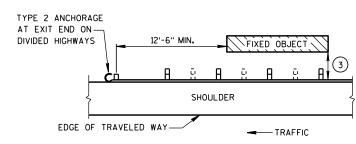
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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2014
DATE
FHWA

DATE
FOR THE PROPOSED PROBLEM OF THE PROBLEM OF THE

BEAM GUARD AT SIDEROADS OR DRIVEWAYS



BEAM GUARD AT OBSTACLES EXIT END - ONE WAY TRAFFIC

GENERAL NOTES

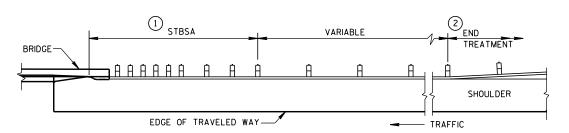
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE PERTINENT STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

W6 X 9 OR W6 X 8.5 STEEL POSTS WITH NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6" X 8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS.

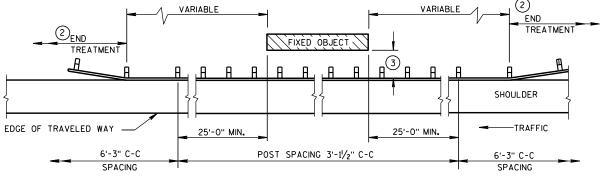
THE LOCATIONS AND LENGTHS OF BEAM GUARD ARE SHOWN ELSEWHERE IN THE PLAN.

- (1) STEEL THRIE BEAM STRUCTURAL APPROACH (STBSA) SEE CURRENT SDD 14B20.
- 2 USE AN APPROVED END TREATMENT FOR THE TRAFFIC APPROACH SIDE OF BRIDGE/OBSTACLES. USE TYPE 2 ANCHORAGE ONLY AT THE DOWNSTREAM ENDS OF BEAM GUARD LOCATED ALONG ROADWAYS WITH ONE WAY TRAFFIC.

3	MINIMUM LATERAL DISTANCE FROM FACE OF BEAM GUARD TO FIXED OBJECT	POST SPACING
	3'-6"	3' - 11/2"
	4'-6"	6' - 3"

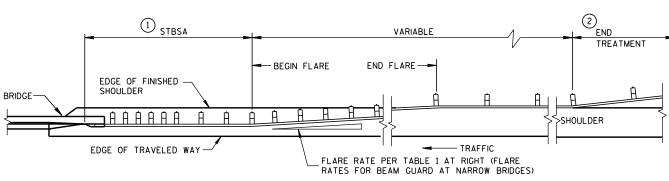


BEAM GUARD AT FULL WIDTH BRIDGES



BEAM GUARD AT OBSTACLES - TWO WAY TRAFFIC

(RAIL TO OBSTACLE CLEARANCE 3'-6" TO 4'-6")



BEAN	M GUARD	AT	NAR	ROW E	RID	GES
(FLARED TO	SHOULDER	EDGE,	THEN	PARALLE	L TO	ROADWAY)

TABLE 1
FLARE RATES FOR BEAM
GUARD AT NARROW BRIDGES

POSTED SPEED (MPH)	FLARE RATE
25	13:1
30	15:1
35	16:1
40	18:1
45	21:1
50	24:1
55	26:1
65	30:1

STEEL PLATE BEAM GUARD CLASS "A" AT BRIDGES, OBSTACLES AND SIDEROADS/DRIVEWAYS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED	
8-21-07	/S/ Jerry H.Zogg
DATE	ROADWAY STANDARDS DEVELOPMENT
FHWΔ	ENGINEER

6

S.D.D.

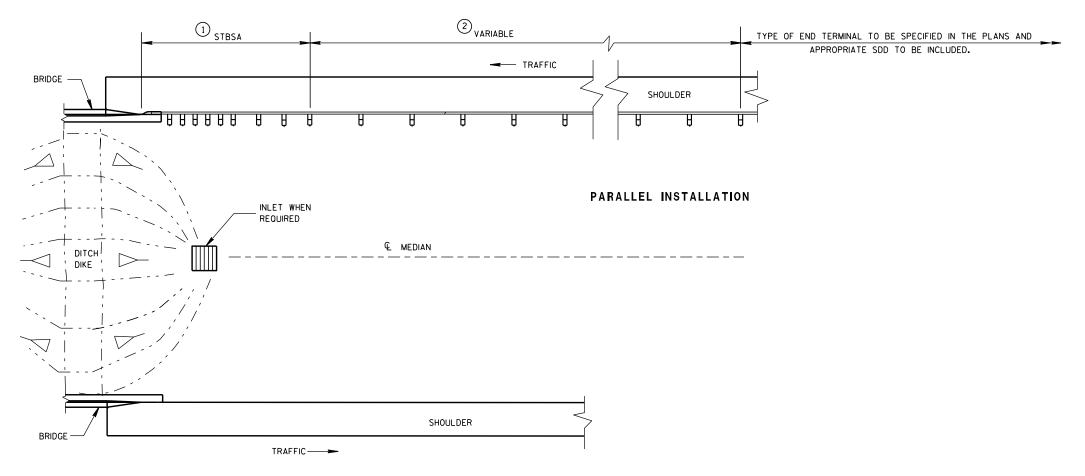
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D.D. 14 B 18

- 1 STEEL THRIE BEAM STRUCTURAL APPROACH (STBSA) SEE CURRENT SDD 14B20.
- 2 LOCATIONS AND LENGTHS OF BEAM GUARD ARE SHOWN ELSEWHERE IN THE PLAN.



BEAM GUARD AT MEDIAN APPROACH TO BRIDGES

STEEL PLATE BEAM GUARD CLASS "A" AT MEDIAN APPROACH TO BRIDGES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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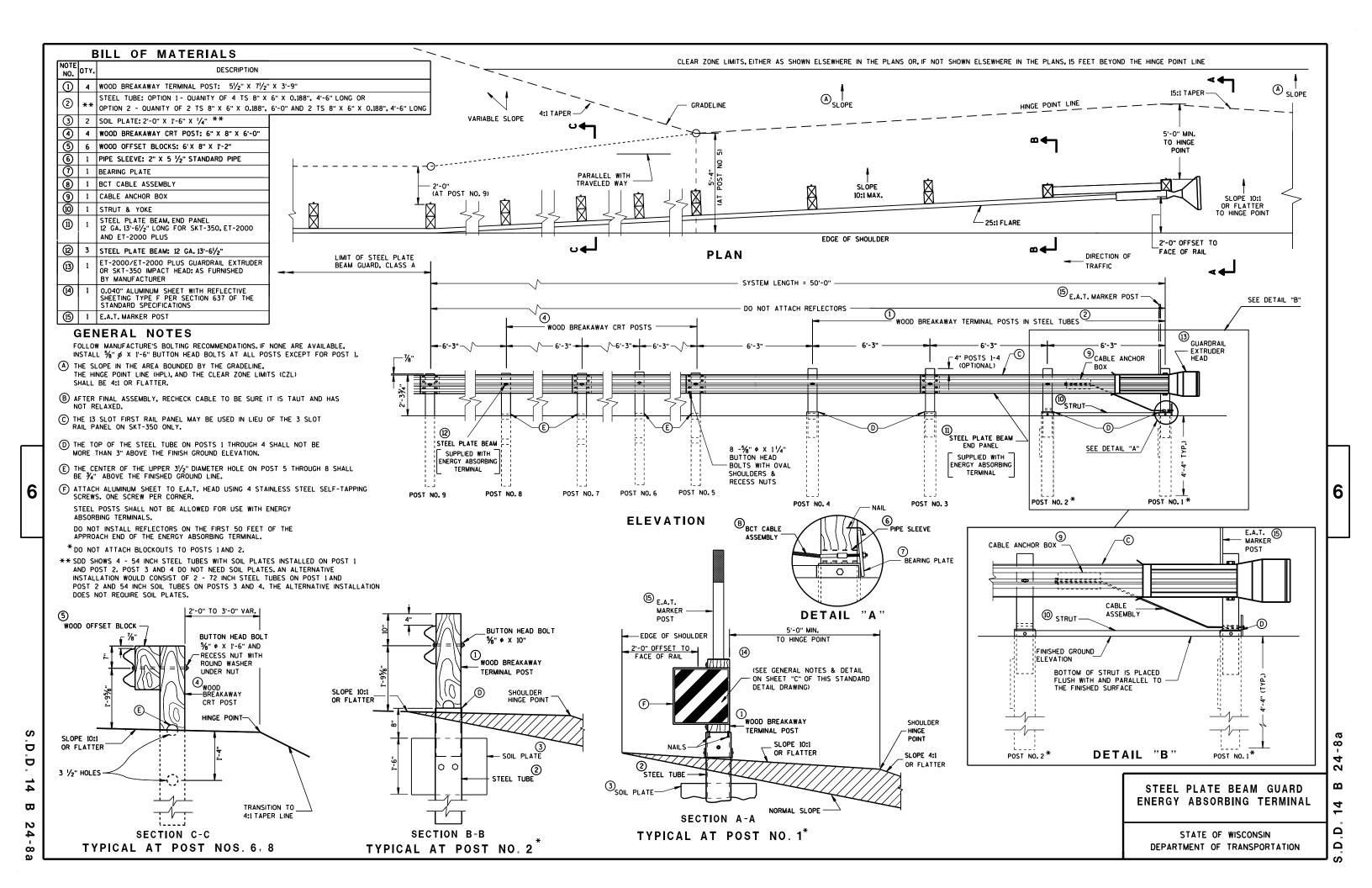
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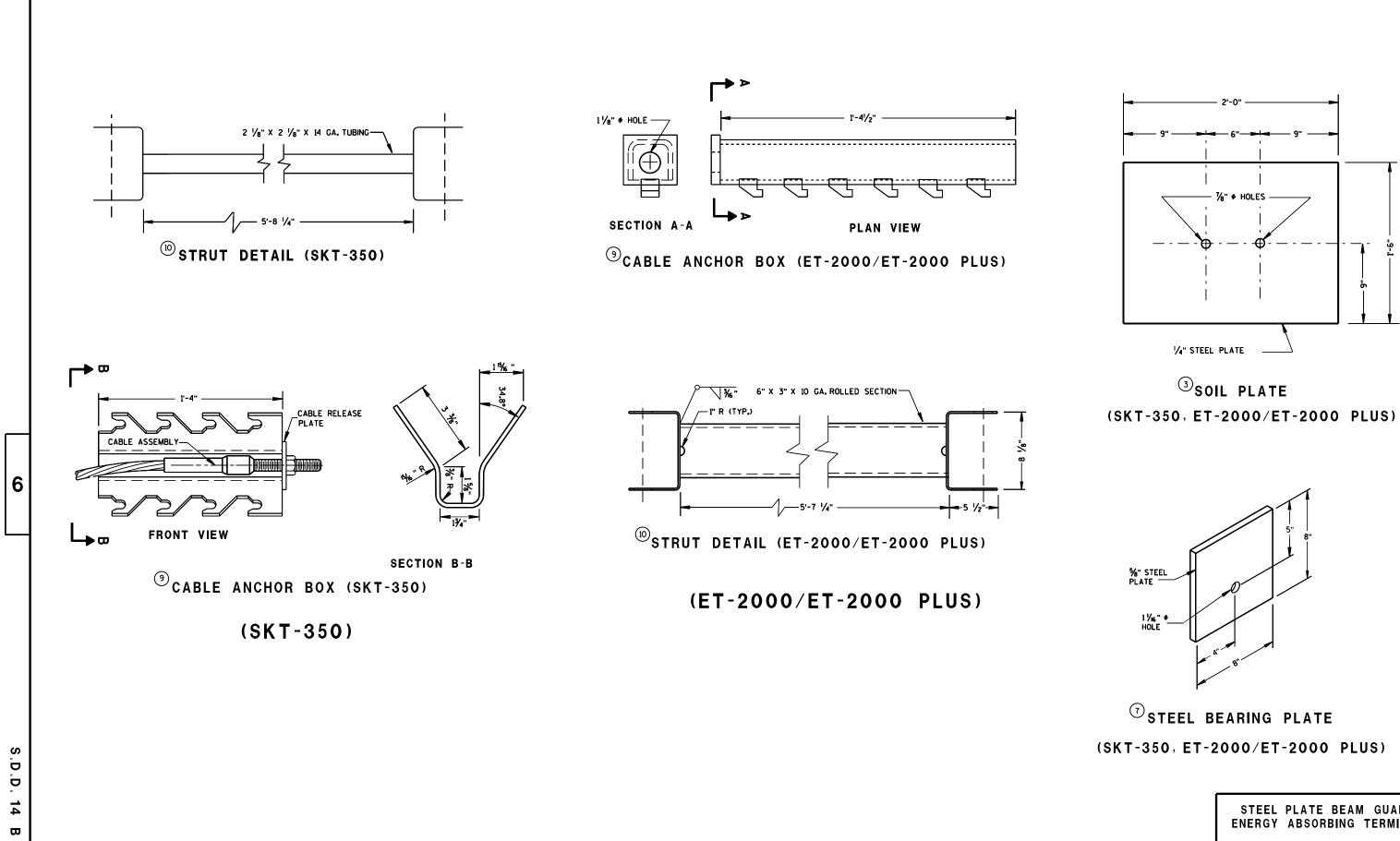
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APPROVED

8-21-07 /S/ Jerry H. Zogg
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

S.D.D. 14 B 18-6k

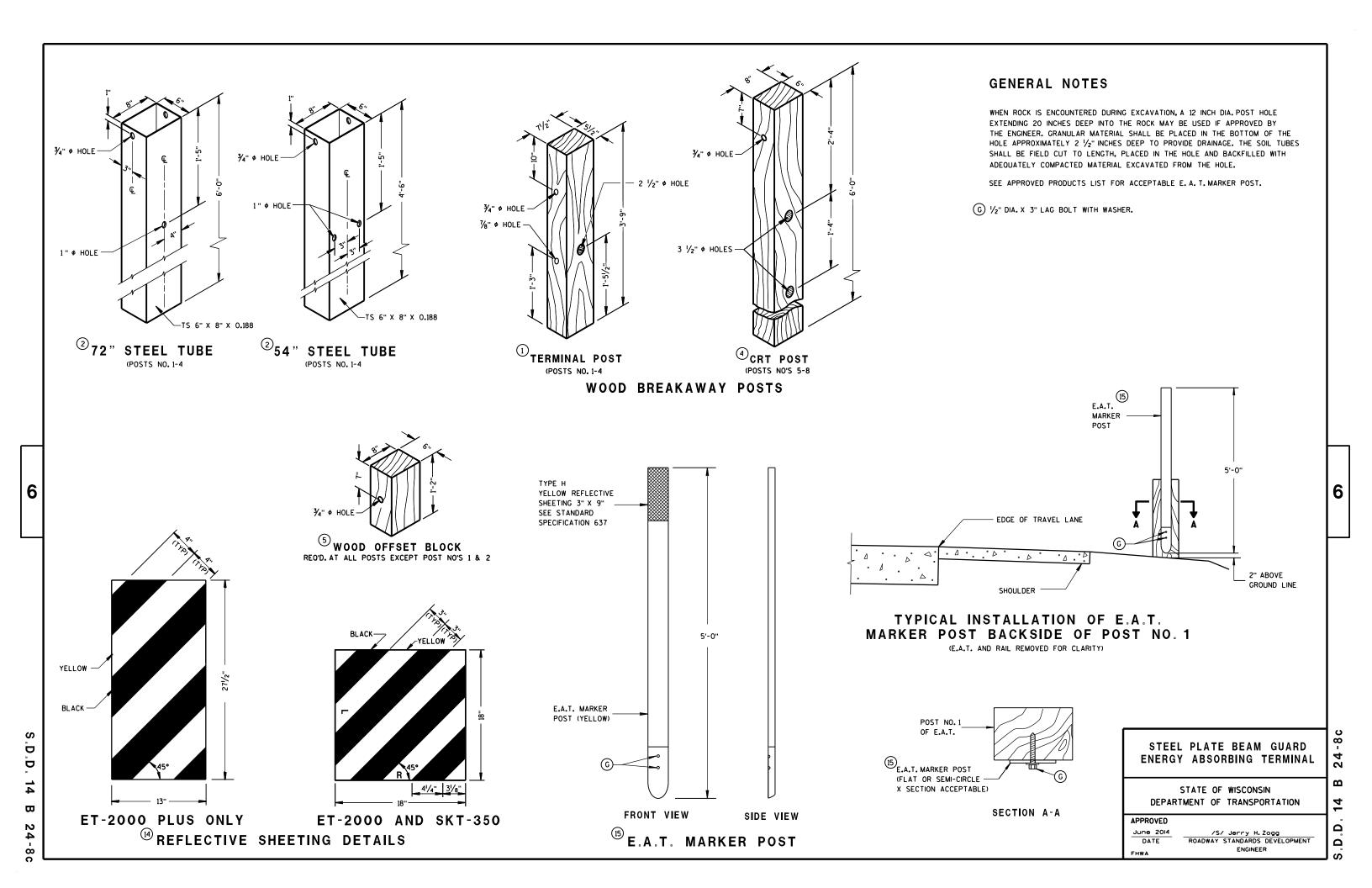




24-8b

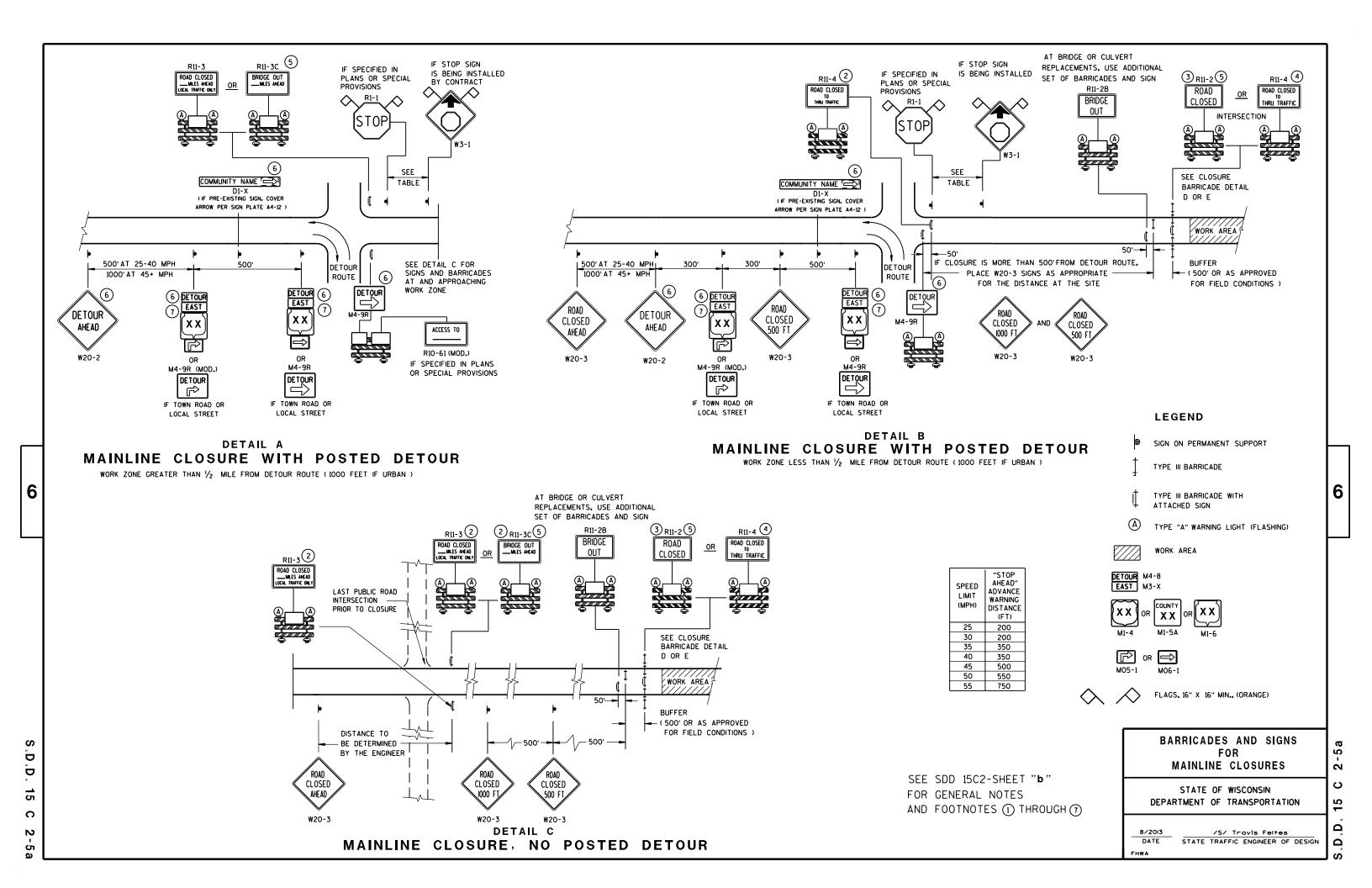
STEEL PLATE BEAM GUARD **ENERGY ABSORBING TERMINAL**

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION 14 أ يُ





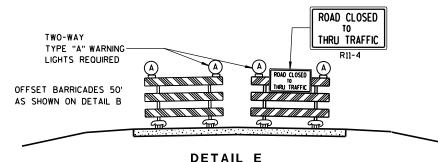




BRIDGE ROAD 1)TWO-WAY **CLOSED** TYPE "A" WARNING LIGHTS REQUIRED OUTSIDE EDGE OF SHOULDER OUTSIDE EDGE OF SHOULDER OR FACE OF CURB OR FACE OF CURB **DETAIL D**

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30". R11-3, R11-4 AND R10-61 SHALL BE 60" X 30". M4-9 SHALL BE 30" X 24". M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.) M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)

M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.) MO5-1 AND MO6-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.) D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS. R1-1 SHALL BE 36" X 36".

- (1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT
- THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS. PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

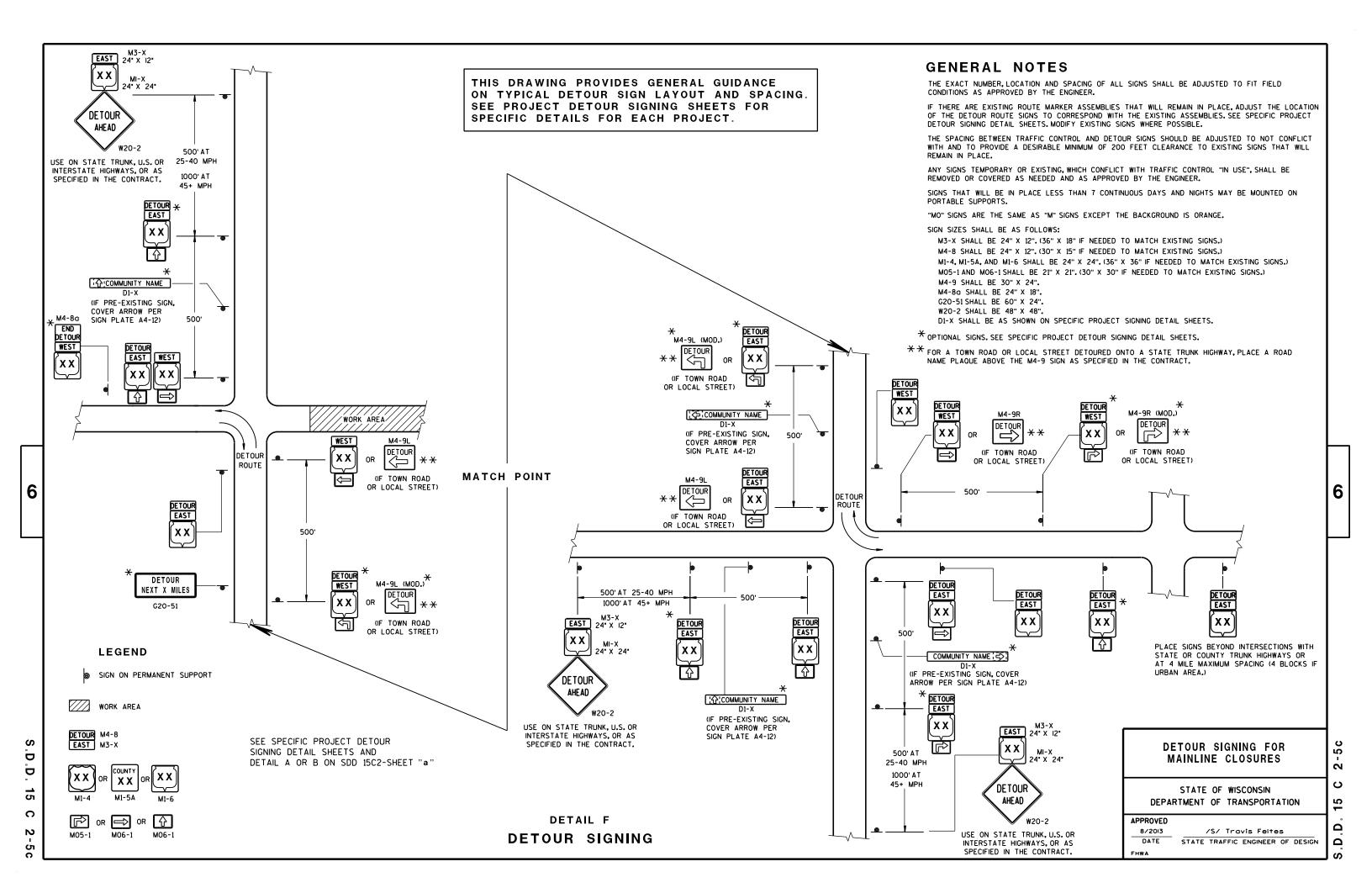
BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN

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GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
RI1-2 SHALL BE 48" X 30".
RI1-4 AND RI1-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

SIGN ON PERMANENT SUPPORT

TYPE III BARRICADE

TYPE III BARRICADE WITH
ATTACHED SIGN

(A) TYPE "A" WARNING LIGHT (FLASHING)

//// w

WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

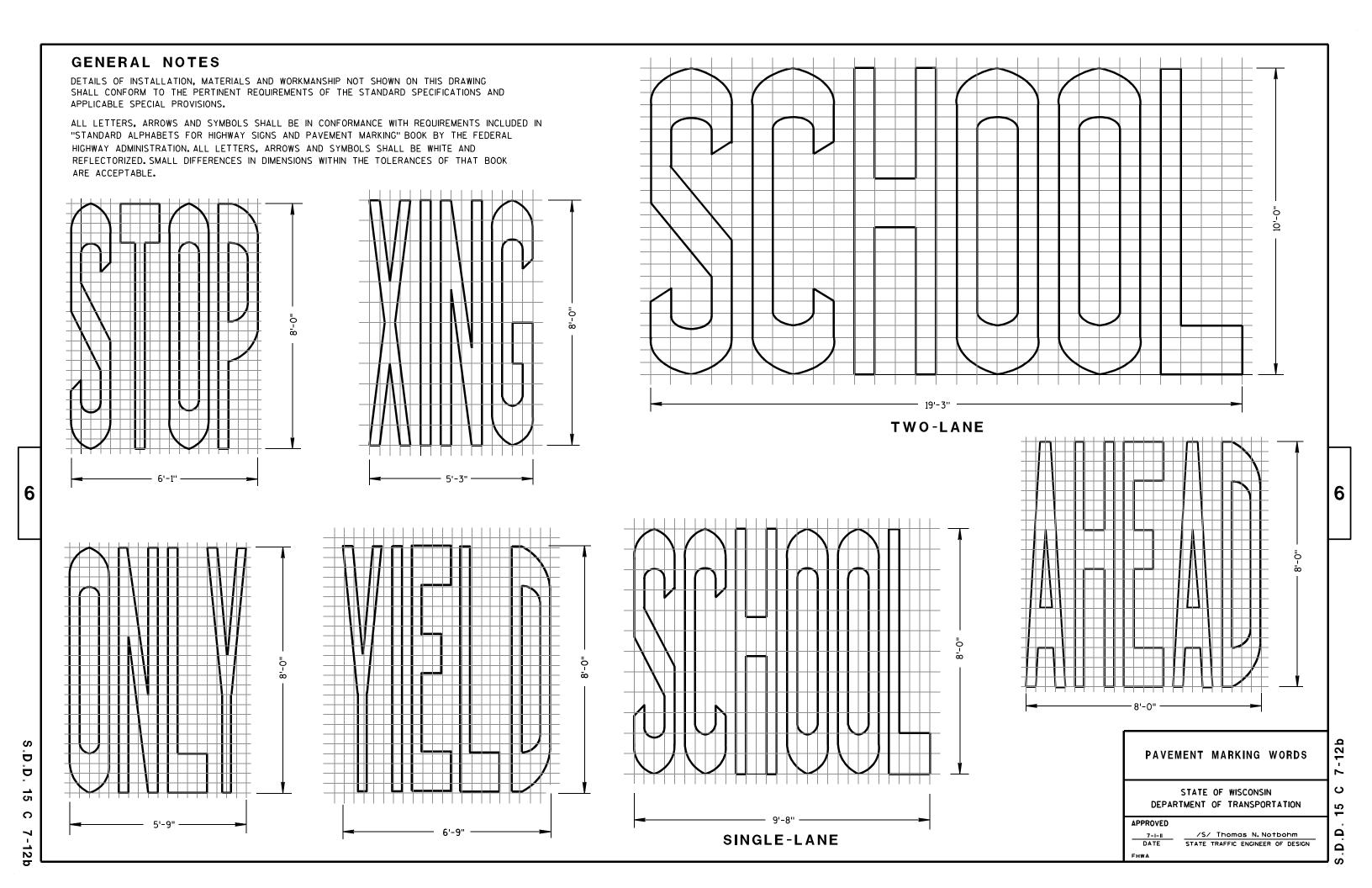
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

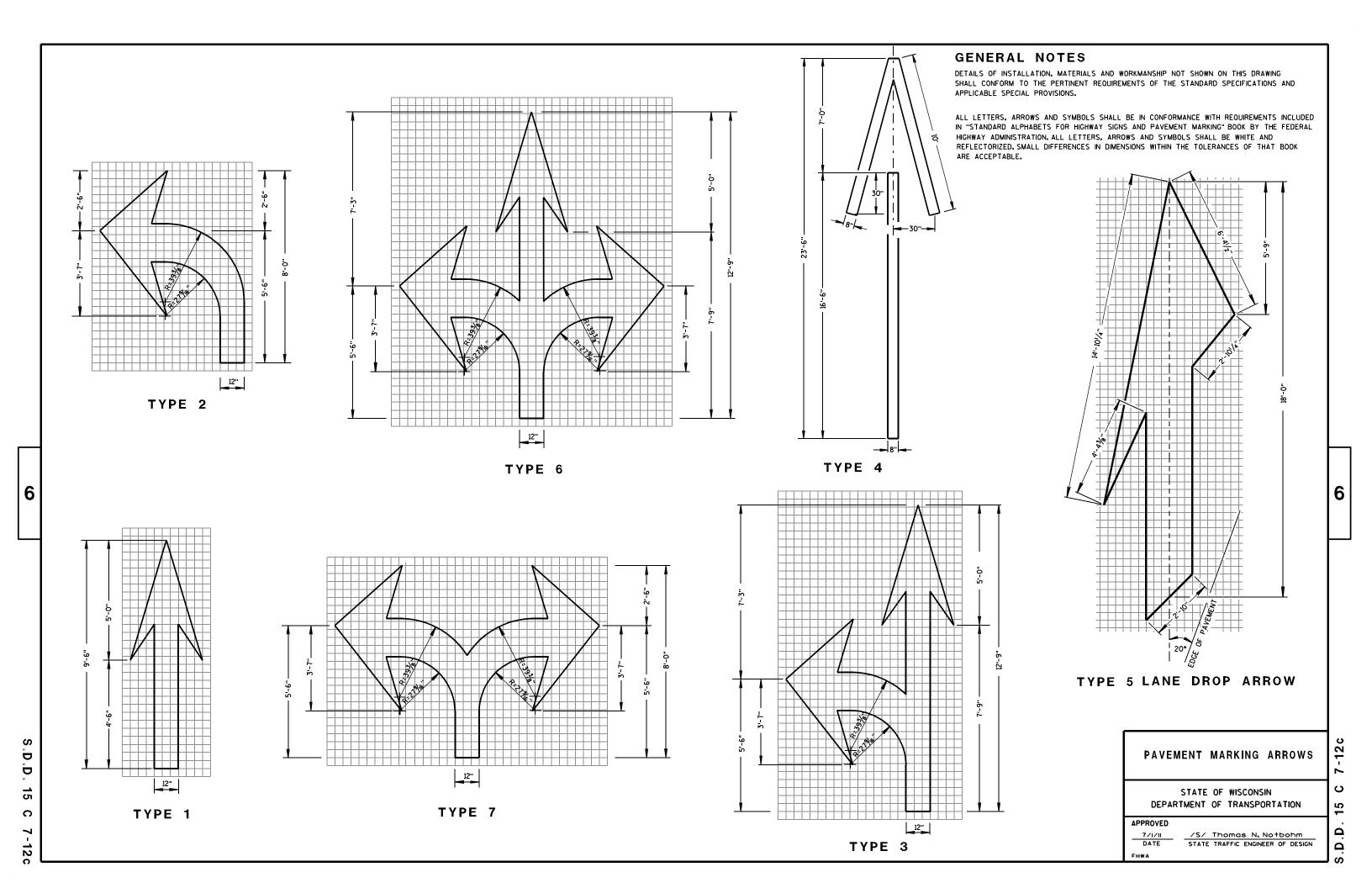
APPROVED

8/2013 /S/ Travis Feltes

DATE STATE TRAFFIC ENGINEER OF DESIGN

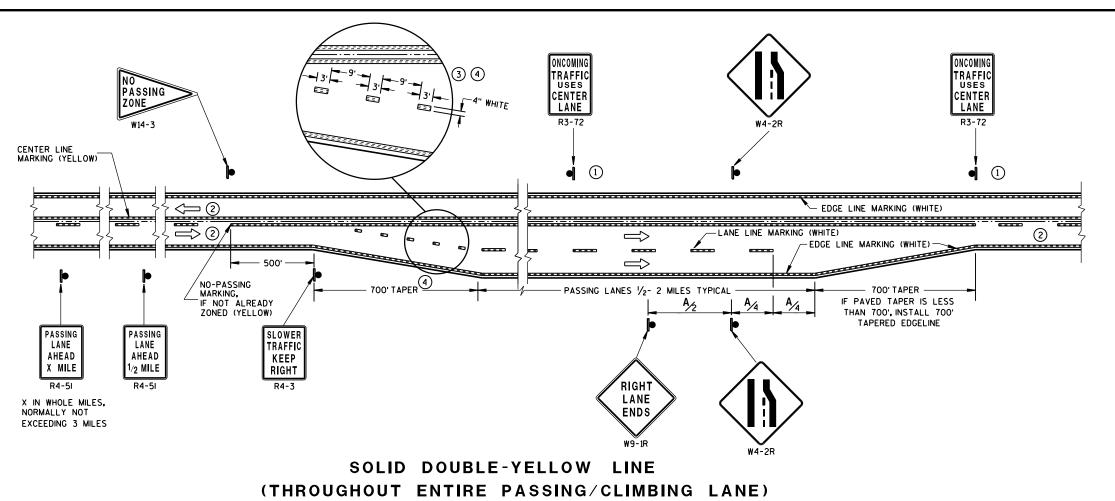
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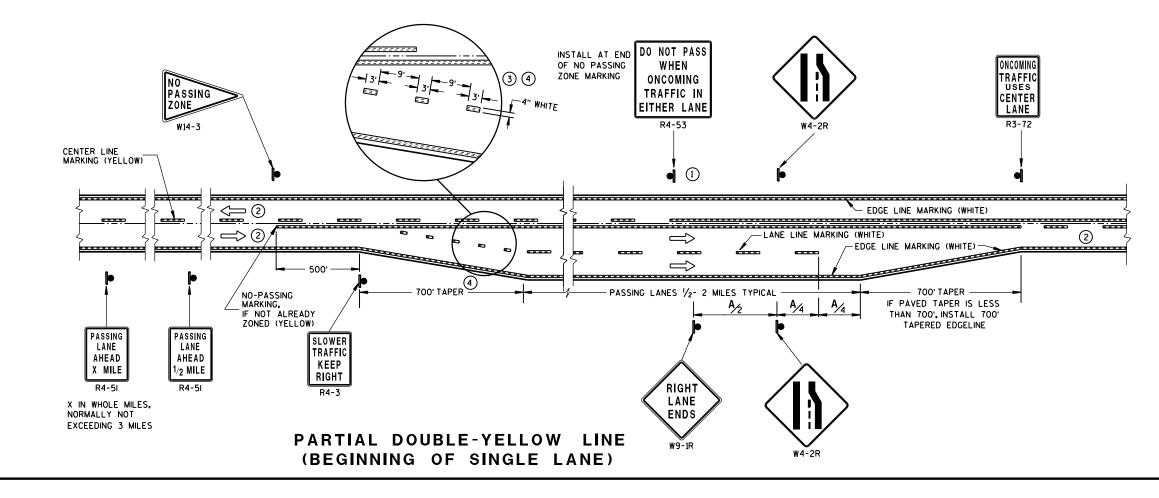


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GENERAL NOTES

- $\ensuremath{\bigcirc}$ Sign shall be repeated at 1 mile increments or at the discretion of the regional traffic engineer.
- (2) THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- 3 THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3'LINE 9'GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL (>>) SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
45	750
50	850
55	950

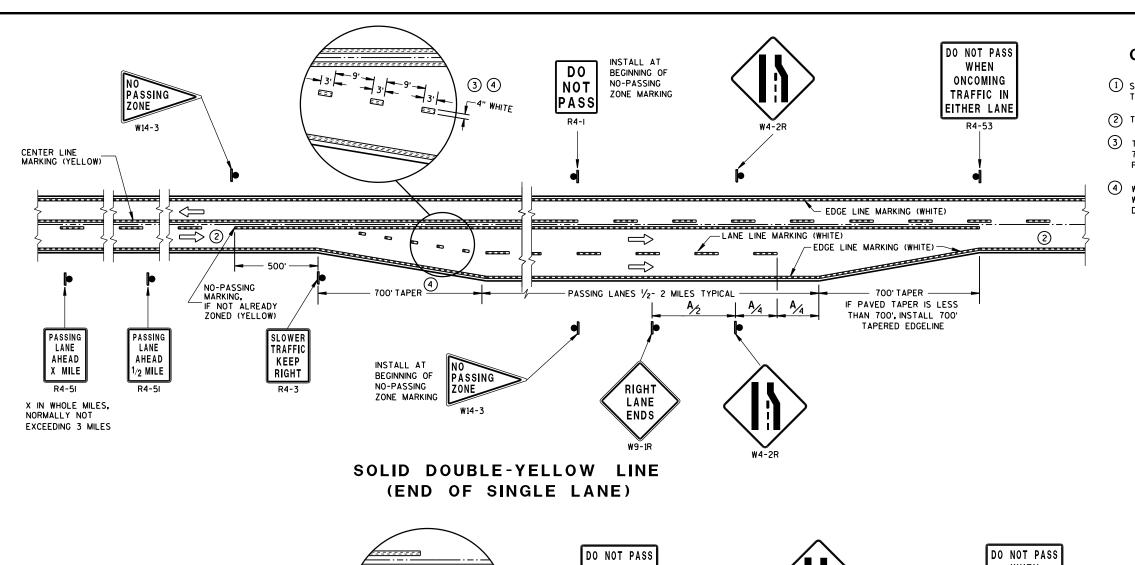
PAVEMENT MARKING & SIGNING
(CLIMBING LANE &
PASSING LANE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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5 C 8-16c

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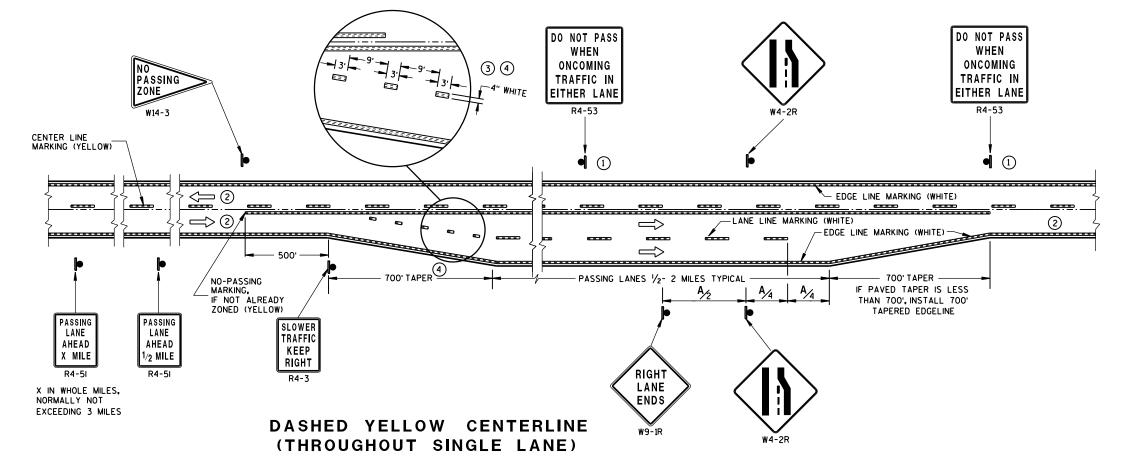
GENERAL NOTES

- \bigodot Sign shall be repeated at $\ensuremath{\mathcal{V}}_2$ mile increments or at the discretion of the regional traffic engineer.
- (2) THERE MAY BE SOLID YELLOW ON THE CENTERLINE DUE TO SIGHT CONDITIONS.
- THE TAPER LENGTH OF THE DOTTED LINE PAVEMENT MARKING SHALL BE 700 FEET, 3'LINE 9'GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- WHEN THE ENTRANCE TAPER IS LESS THAN 700 FEET OR THE SHOULDER WIDTH IN THE PASSING/CLIMBING LANE IS LESS THAN THE ADJACENT HIGHWAY, DO NOT INSTALL DOTTED LINE PAVEMENT MARKING.

ARROW SYMBOL () SHOWS DIRECTION OF TRAVEL

DISTANCE TABLE

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45	750
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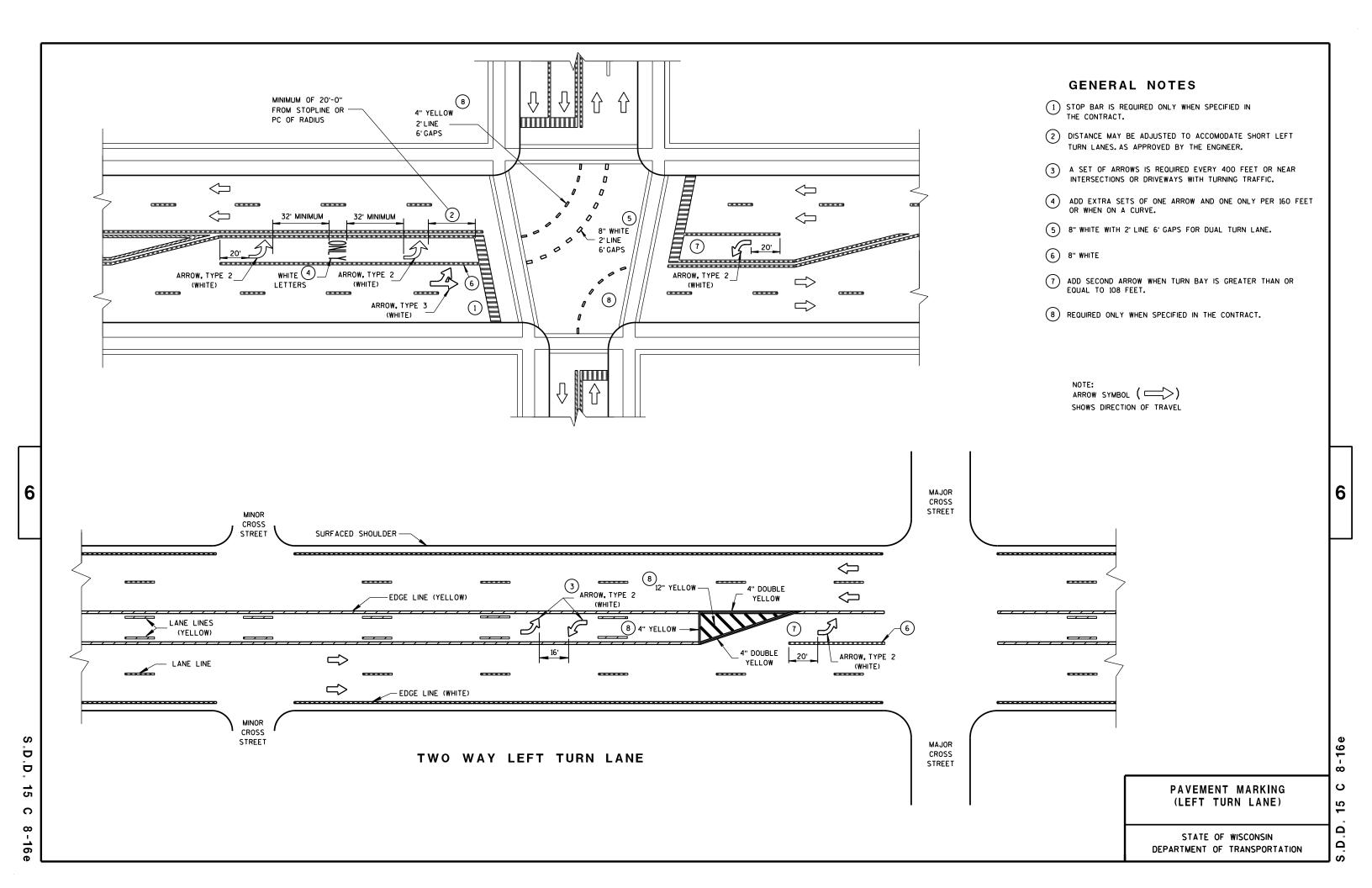


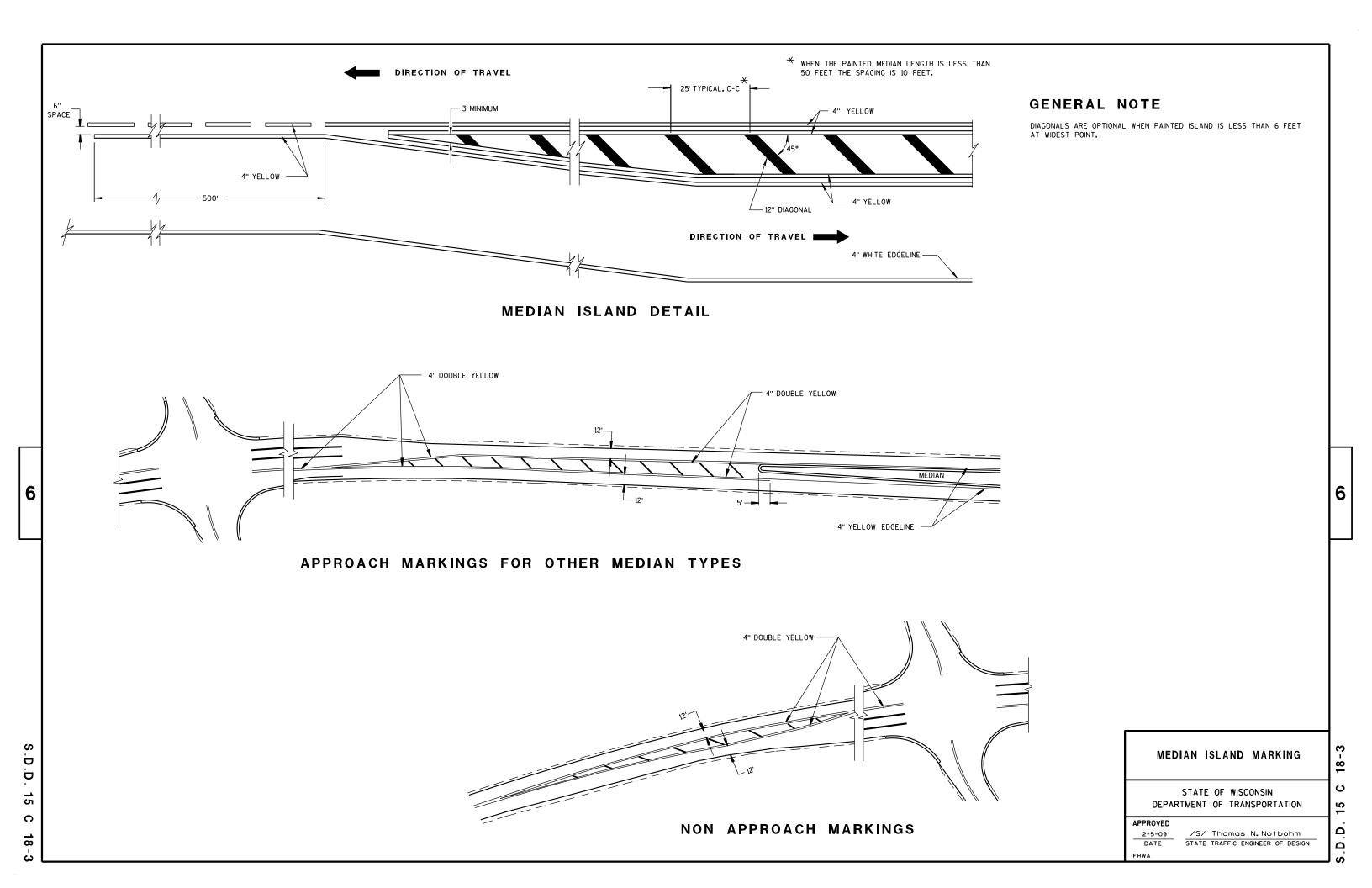
PAVEMENT MARKING & SIGNING (CLIMBING LANE & PASSING LANE)

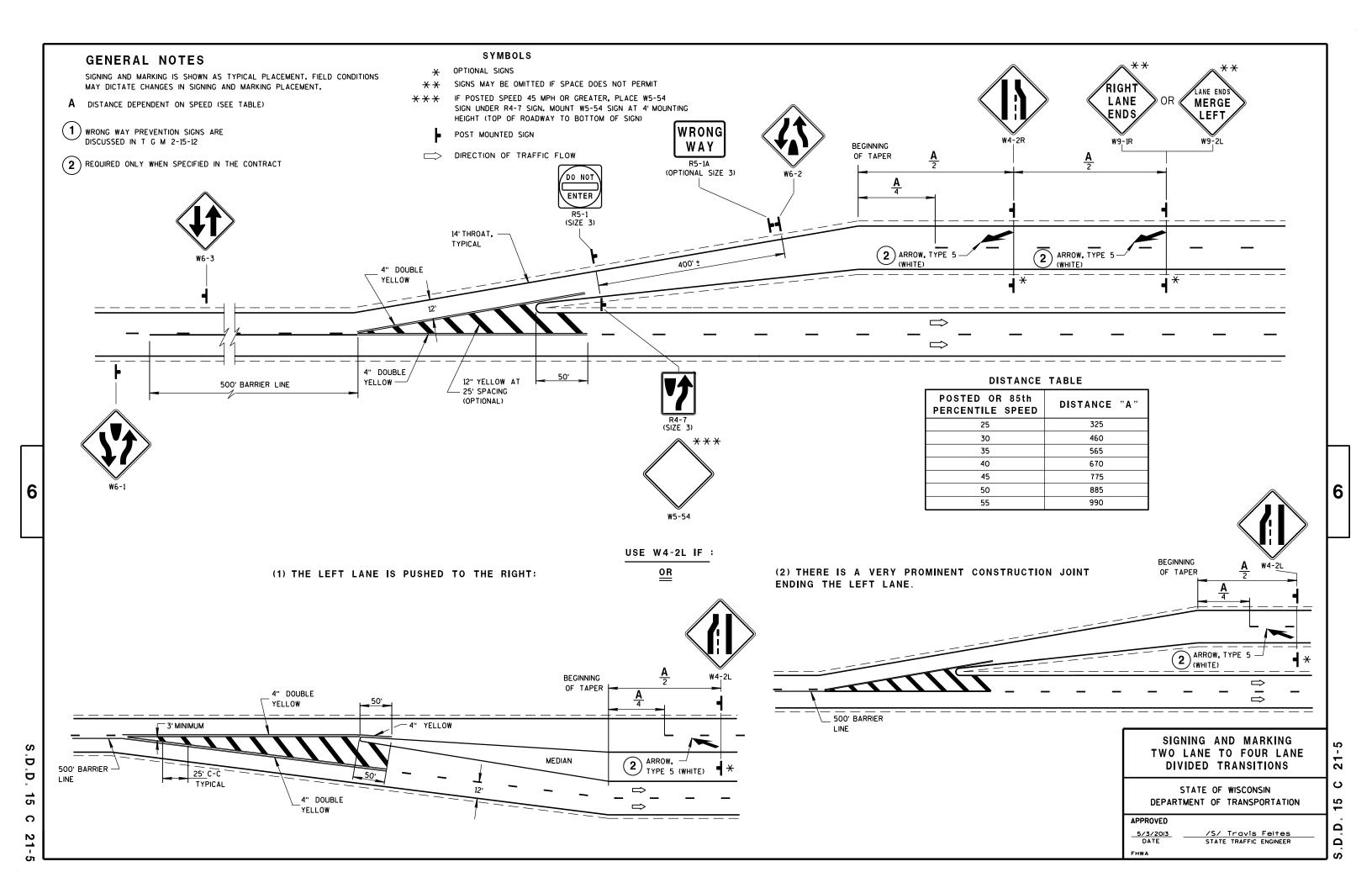
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

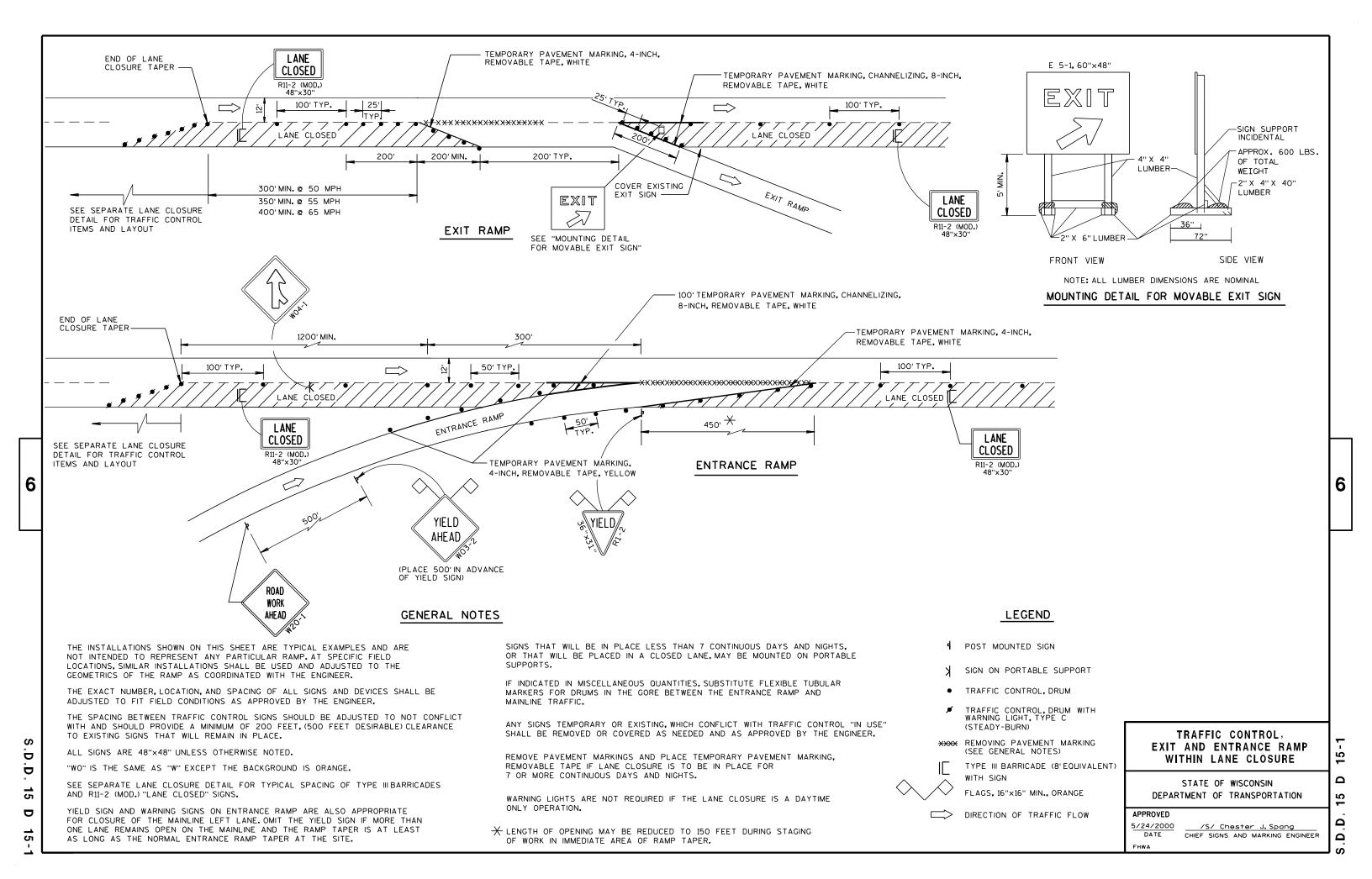
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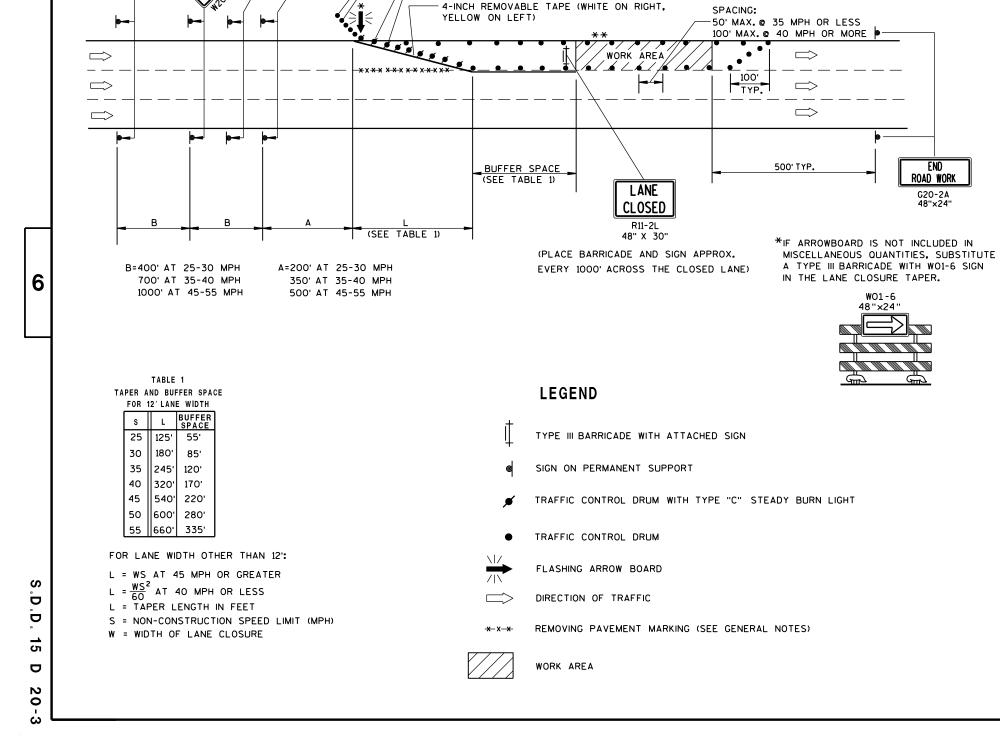












(5) DRUMS SPACED @ 10'

INTERVALS AS NEEDED IN

FRONT OF ARROW BOARD

25'@ 35 MPH OR LESS

50'@ 40 MPH OR MORE

TEMPORARY PAVEMENT MARKING.

SPACING:

ROAD WORK

NEXT___MILES

G20-1

60" X 24"

CLOSED

AHEAD

AHEAD

GENERAL NOTES

**THE LINE OF DRUMS SHOWN ALONG THE MEDIAN/CENTERLINE

ADJACENT TO THE WORK AREA. FOR THIS CONDITION INSTALL

W20-1 "ROAD WORK AHEAD" SIGN FOR OPPOSING DIRECTION OF

IS REQUIRED ONLY WHERE THERE IS OPPOSING TRAFFIC

TRAFFIC, IN ADVANCE OF THE WORK AREA.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"×48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W2O-1, G2O-1 AND G2O-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

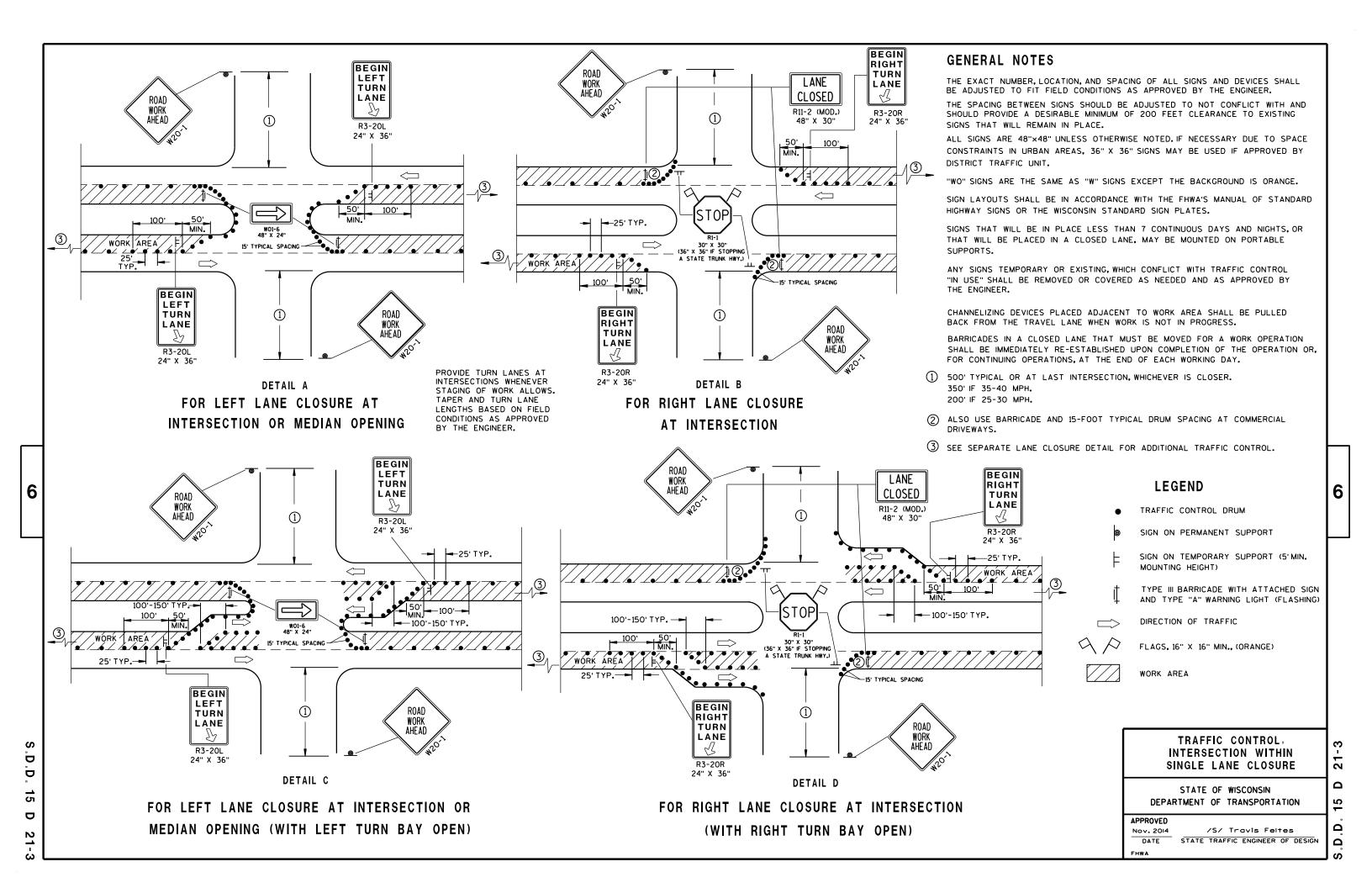
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

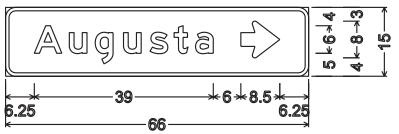
TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

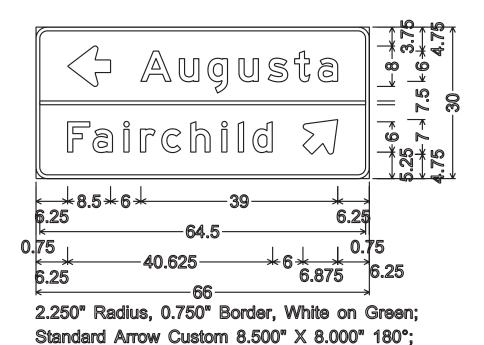
APPROVED
Feb. 2015
DATE
STATE TRAFFIC ENGINEER OF DESIGN

S.D.D. 15 D 2

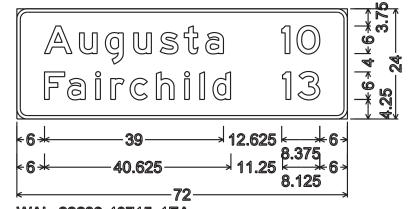




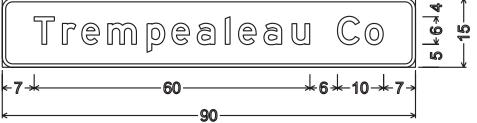
2.250" Radius, 0.750" Border, White on Green; "Augusta" E; Standard Arrow Custom 8.500" X 8.000" 0°;



Standard Arrow Custom 8.500" X 8.000" 45°:



WAL 22689-40715 1EA: 2.250" Radius, 0.750" Border, White on Green; "Augusta" E; "10" E; "Fairchild" E; "13" E;



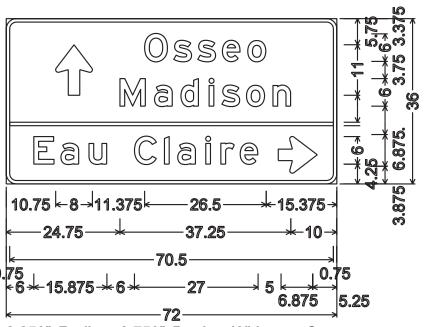
2.250" Radius, 0.750" Border, White on Green; "Trempealeau" E; "Co" E;

NOTES

- 1. All Signs Type II Type H Reflective
- 2. Color:

Background - GREEN Message - WHITE

3. Message Series - E



2.250" Radius, 0.750" Border, White on Green; Standard Arrow Custom 11.000" X 8.000" 90°; "Osseo" E; "Madison" E; "Eau" E; "Claire" E; Standard Arrow Custom 8.500" X 8.000" 45°;

PROJECT NO: 1520-04-71

HWY: USH 10

COUNTY: TREMPEALEAU

PERMANENT SIGNING

SHEET NO:

FILE NAME : C:\Users\dotjdb\Desktop\5611A115.DGN

"Augusta" E; "Fairchild" E;

PLOT DATE: 09-FEB-2015 14:49

PLOT BY: \$\$...plotuser...\$\$ PLOT NAME:

PLOT SCALE: 15.990000:1.000000

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard

areater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.



PROJECT NO:

J32-1

J22-1

J23-1

J33-1

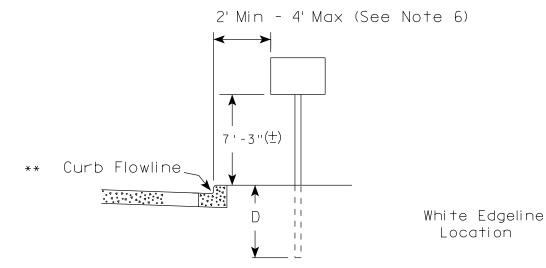
PLOT BY: mscsja

PLATE NO. __A2-15.8

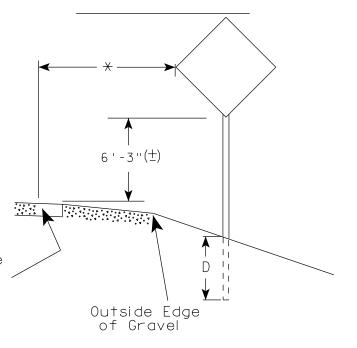
DATE 2/06/14

SHEET NO:

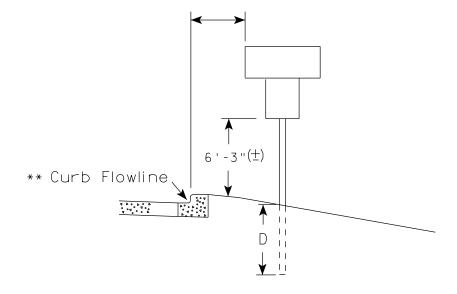
URBAN ARFA



RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



5'-3"(生) A POLICE AND A POL White Edgeline D^{-1} Location Outside Edae of Gravel

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where

there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

PLOT BY: mscsja

GENERAL NOTES

- 1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is $7'-3''(\pm)$ or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A2-1S) is $7'-3''(\pm)$ or $6'-3''(\pm)$ per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is $5' - 3'' (\pm)$.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (+) tolerance for mounting height is 3 inches.
- 8. Folding signs shall be mounted at a height of 5'-3'' (\pm) or as directd by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm) . The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3'' (\pm).

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

SHEET NO:

APPROVED

for State Traffic Engineer

DATE 11/12/14

PROJECT NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43.DGN COUNTY:

PLOT DATE: 12-NOV-2014 14:03

PLOT NAME :



NOTES: 1. ALL MATERIAL TO BE APPROVED

BY ENGINEER PRIOR TO INSTALLATION

- 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
- 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

HWY:



PLAN VIEW

COUNTY:

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

For State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN

PROJECT NO:

PLOT DATE: 27-JAN-2014 09:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 13.659812:1.000000

APPROVED

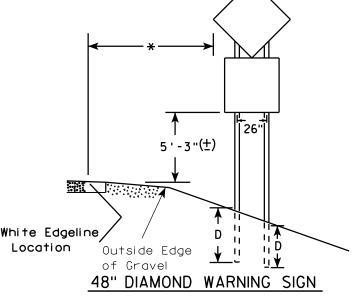
GENERAL NOTES

- 1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- 2. See tables below for required number of posts.
- 3. For expressways and freeways. mounting height is 7'-3'' (±) or 6'-3'' (±) depending upon existence of sub-sign.
- 4. The (±) tolerance for mounting height is 3 inches.
- 5. Minimum mounting height for J assemblies (A2-1S) is 7'-3'' (±) or 6'-3'' (±) per urban or rural detail respectively.
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
- 8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8). Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).
- * 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.
- ** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.
- *** See A4-3 sign plate for signs 4' or less in width or less than 20 S.F. in area.

URBAN AREA RURAL AREA (See Note 3) 2'Min - 4'Max (See Note 6) ₩E# FF# 6'-3"(±) 6'-3"(±) 7'-3"(±) ** Curb ********\ Flowline D **7000** White Edgeline

2'Min - 4'Max (See Note 6) 6'-3"(±) Curb Flowline. 48" DIAMOND WARNING SIGN

D 11



COUNTY:

Outside Edge

of Gravel

	SIGN SHAPE OTHER THAN (TWO POSTS REQUIRED	
	L	E
* * *	Greater than 48" Less than 60"	12"
	60" to 120"	L/5

SIGN SHAPE OTHER THAN (THREE POSTS REQUIR	
L	E
Greater than 120" less than 168"	12"

HWY:

White Edgeline,

Location

SIGN SHAPE OTHER THAN (FOUR POSTS REQUIRE	
L	E
168" and greater	12"

Location

Outside Edae

of Gravel

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

Matther

PLATE NO. A4-4.13

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A44.DGN

PROJECT NO:

PLOT DATE: 12-NOV-2014 14:01

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 107.021305:1.000000

WISDOT/CADDS SHEET 42

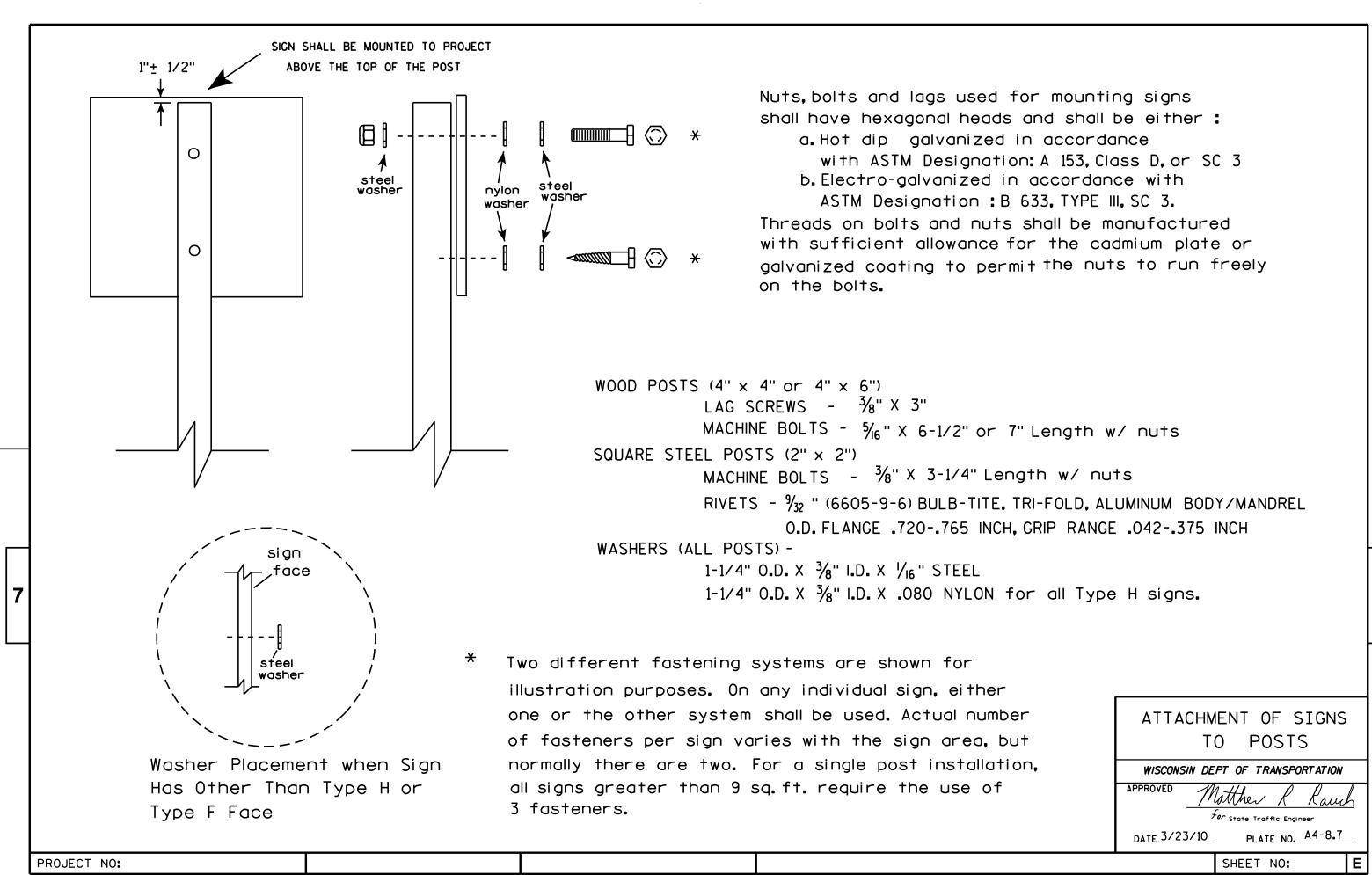
SHEET NO:

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 11/12/14





PROJECT NO: HWY: COUNTY: SHEET NO: FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE: 05-FEB-2015 17:09 PLOT BY: mscsja PLOT NAME : PLOT SCALE: 13.659812:1.000000

DATE 2/05/15

PLATE NO. <u>A4-9.9</u>

For State Traffic Engineer



HWY:

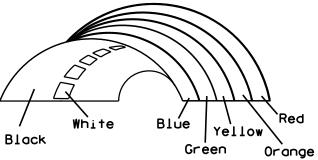
Background Colors of Symbol*

Z F Z

A F X A

₽ 4

* VARIES



*1/4" Black Border between each color of rainbow and border of rainbow

COUNTY:

NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - (See Note 5)

- 3. Message Series (See Note 6)
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Border Blue

Line 1 - Red

Line 2 - Black

Line 3-5 - Blue

6. Line 1 - Dutch 8011L

Line 2 - Series E

Line 3-5 - Series C

7. Contractor shall provide and install a new post bracket in accordance with the I55-56B sign detail.

STANDARD SIGN I55-56

For State Traffic Engineer

DATE 4/27/11 PLATE NO. 15!

ATE 4/27/11 PLATE NO. 155-56.3

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\I5556.DGN

PROJECT NO:

PLOT DATE: 27-APR-2011 10:05

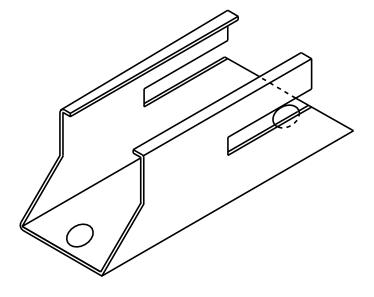
PLOT BY: mscj9h

PLOT NAME :

PLOT SCALE: 7.945391:1.000000

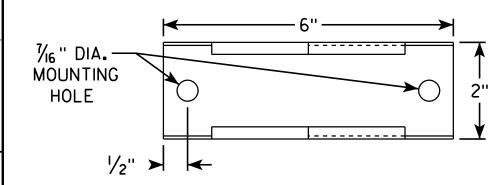
945391:1.000000 WISDOT/CADDS SHEET 42

ISOMETRIC VIEW

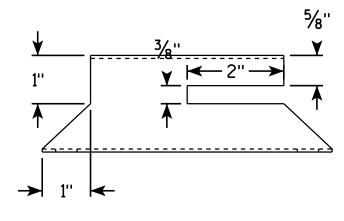


TOP VIEW

HWY:



SIDE VIEW

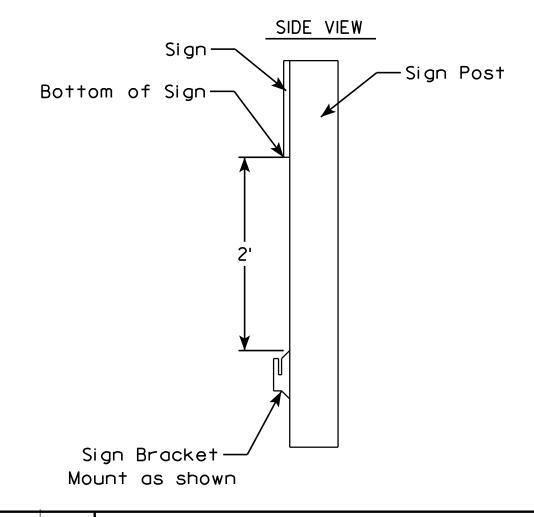


← 2" →

END VIEW

NOTES

- 1. Must be capable of permanent attachment to a wood or steel channel sign post utilizing the fastening hardware specified on the A4-8 sign plate.
- 2. Shall be entirely primed and painted with two coats of a black powder coated enamel paint.
- 3. Shall be made with 12 gauge steel, and incorporate no welds, no hinged components, no threaded lock-type components, and no parts which are loose or can be separated from the main body.
- 4. Shall have rounded edges with at least $\frac{1}{8}$ " radii.
- 5. Shall not have unrounded and uncoated metaledges which can contact the back surface of the roll-up sign.
- 6. Top of bracket shall be mounted 2' below the bottom of the 155-56 sign.
- 7. Cost of bracket and fastening hardware shall be incidental to the 155-56 sign.



ROLLUP SIGN BRACKET I55-56B

WISCONSIN DEPT OF TRANSPORTATION APPROVED

for State Traffic Engineer DATE 2/5/10 PLATE NO. 155-56B.1

SHEET NO:

COUNTY:

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE: 1.986348:1.000000

PROJECT NO:

- 1. Sign is Type II See Note 6 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Top Red - Bottom Blue (See Note 6) Message - White - See Note 6

- 3. Message Series See note 5
- 4. Substitute appropriate numerals & ajust spacing as per plate A10-1.
- 5. M1-1 Numerals D Interstate - C

M1-1A - All copy - C

6. Permanent Signs

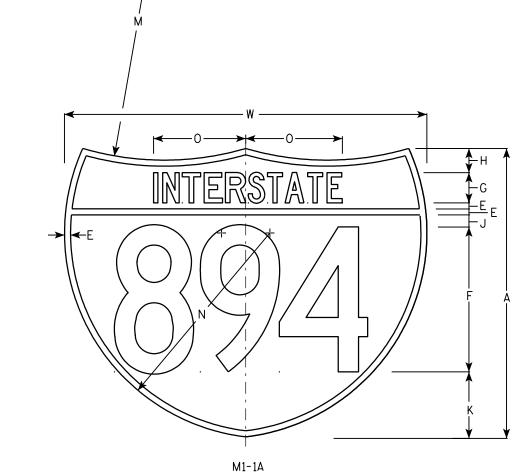
Message - Type H Reflective

Detour or other temporary signs Background - Reflective

Message - Reflective

M1-1

HWY:



PLOT DATE: 13-OCT-2005 14:49

Metric equivalent for these signs are:

SIZE	M1-1	SIZE	M1-1A
1			
2	600 mm X 600 mm	2	600 mm X 750 mm
3	900 mm X 900 mm	3	900 mm X 1125 mm
4	900 mm X 900 mm	4	900 mm X 1125 mm
5	900 mm X 900 mm	5	900 mm X 1125 mm

<u></u>	100	, ,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	X 300	וווווו		J00 I	IIIII	123 11111	<u>' </u>																	M1 - 1	W1-1A	M1-1	W1-1A
SIZE	Α	В	С	D	E	F	G	Ι	I	J	K	L	М	N	0	Ρ	a	R	S	T	U	٧	W	Х	Y	Area sq. ft.	Area sq. ft.	Area m2	Area m2
1																													
2	24				1/2	12	2 ½	2		1	5 ½	15	24	17	7 1/8								30			3.13	3.91	. 36	.46
3	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 ½	36	25 ½	11 ¾								45			7.03	8.79	. 81	1.05
4	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 ½	36	25 ½	11 ¾								45			7.03	8.79	.81	1.05
5	36				3/4	18	3 3/4	3		1 1/2	8 1/4	22 1/2	36	25 1/2	11 ¾								45			7.03	8.79	. 81	1.05

COUNTY:

INTERSTATE ROUTE MARKER M1-1 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

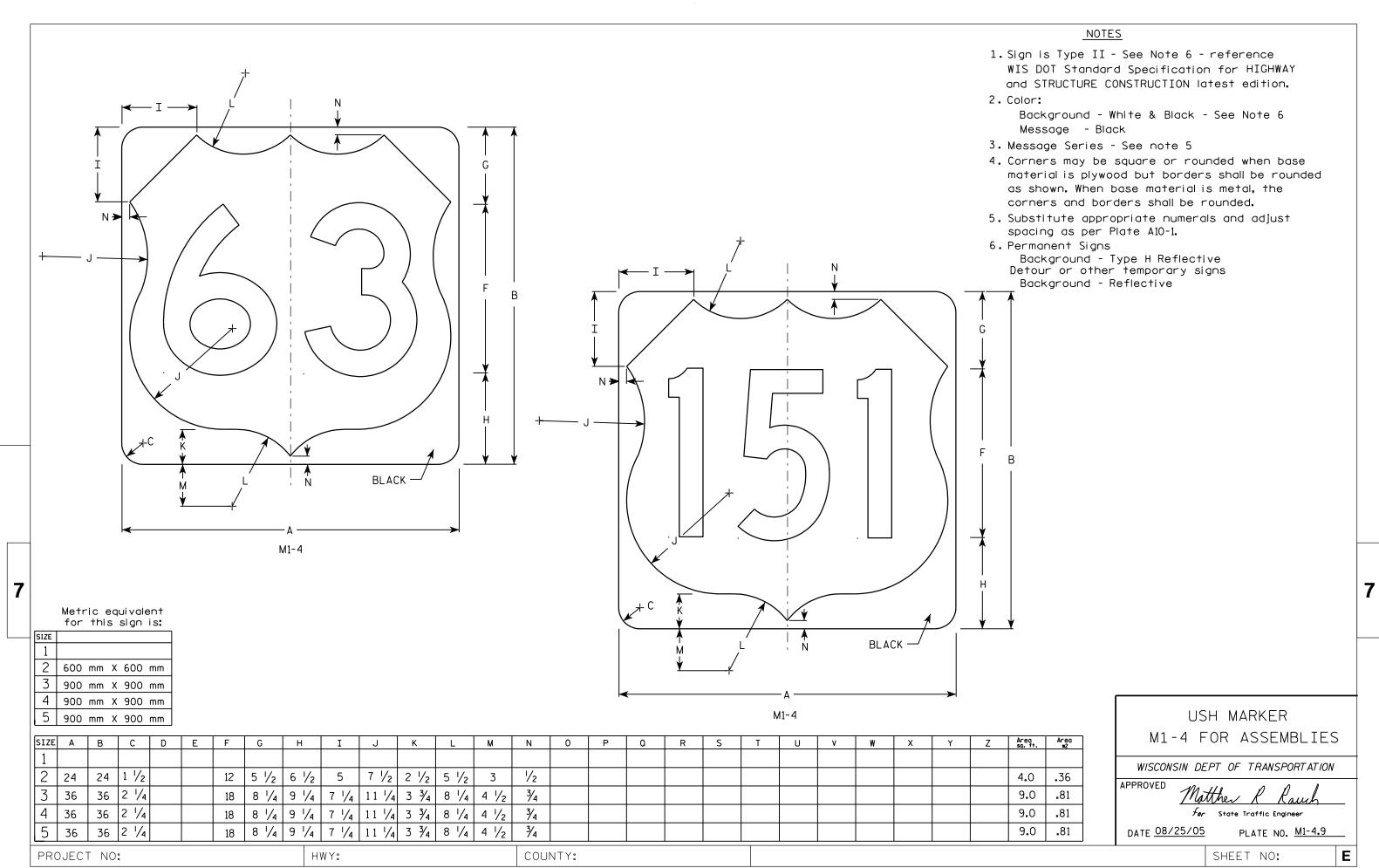
DATE 08/23/05

For State Traffic Engineer

SHEET NO:

PLOT BY : DITJPH PLOT NAME :

PROJECT NO:



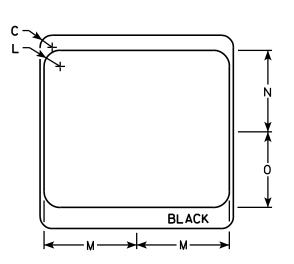
FILE NAME : C:\Users\Projects\tr_stdplate\M14.DGN

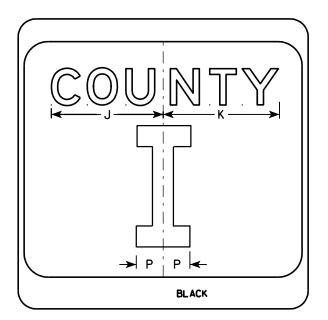
- 1. Sign is Type II see Note 7 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

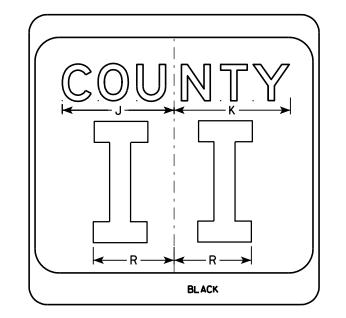
Background - White & Black - See Note 7 Message - Black

- 3. Message Series see Note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Message Series E for 1 letter. Message Series D for 2 letters unless message is too big then Series C. Message Series C for 3 letters unless message is too big then Series B.
- 6. Substitute appropriate letters & optically center to achieve proper balance.
- 7. Permanent Signs

Background - Type H Reflective Detour or temporary Signs Background - Reflective







SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 %	2	11 1/2	10 1/8	9 %	2 1/4		6 %									4.0
3	36		2 1/4			16	4	7 5/8	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 %		10									9.0
5	36		2 1/4			16	4	7 %	5 %	12 1/4	12 1/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
																			_								

COUNTY:

CTH MARKER M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

FerState Traffic Engineer PLATE NO. M1-5A.8 DATE 9/27/11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M15A.DGN

PROJECT NO:

BLACK

HWY:

M1-5A

PLOT DATE: 29-SEP-2011 11:25

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 5.959043:1.000000



- 1. Sign is Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M2-1 Background White

Message – Black

MB2-1 Background - Blue

Message - White

MK2-1 Background - Green

Message - White

MM2-1 Background - White

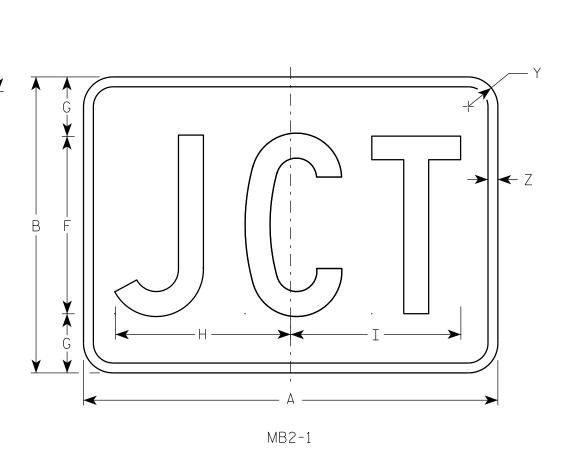
Message - Green

MN2-1 Background - Brown

Message - White

MR2-1 Background - Brown

Message - Yellow



7

SIZE G Н Ν 0 Α 1 1/8 3/8 8 1/8 8 5/8 1 1/2 1/2 3/8 21 15 9 2.20 3 30 21 1 1/8 3/8 3/8 13 12 $\frac{7}{8}$ 12 $\frac{3}{8}$ $1 \frac{1}{2}$ 1/2 4.40 12 $\frac{7}{8}$ 12 $\frac{3}{8}$ 4 30 21 1 1/8 3/8 3/8 13 1 1/2 1/24.40 12 $\frac{7}{8}$ 12 $\frac{3}{8}$ 5 3/8 3/8 30 21 1 1/8 13 4 1 1/2 1/2 4.40

COUNTY:

В

STANDARD SIGN

M2 - 1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther & Rauch

DATE <u>6/30/14</u>

PLATE NO. M2-1.11

SHEET NO:

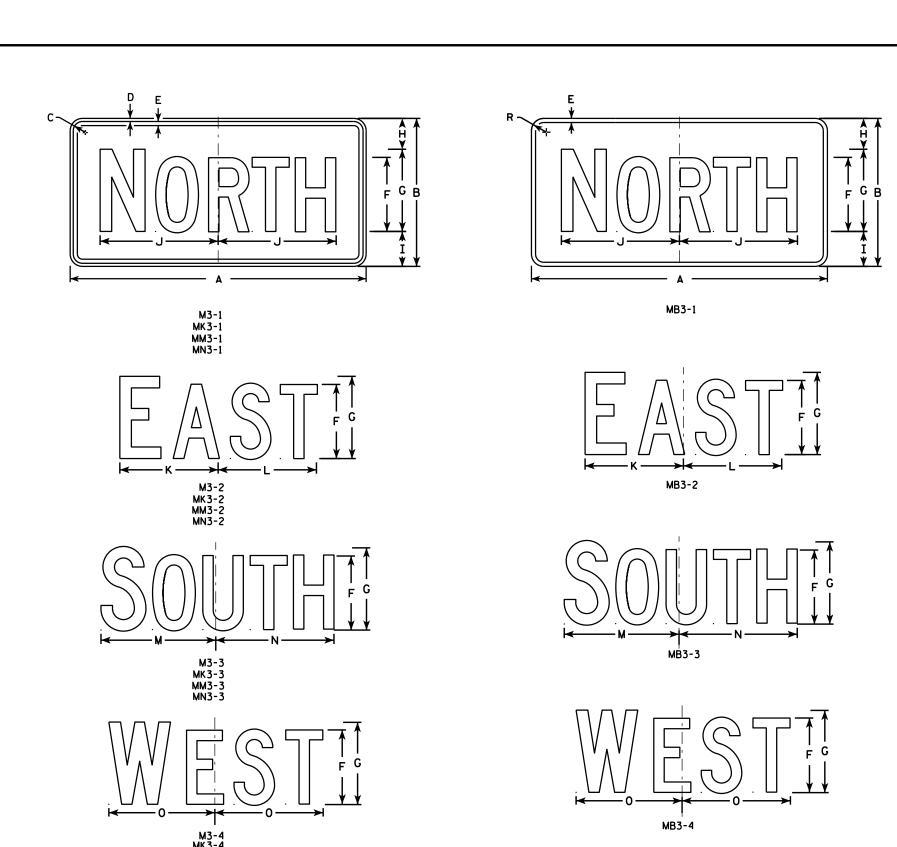
PROJECT NO:

M2-1

MK2-1 MM2-1

MN2-1 MR2-1

HWY:



- 1. All Signs Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M3-1 thru M3-4 Background White

Message - Black

MB3-1 thru MB3-4 Background - Blue

Message - White

MK3-1 thru MK3-4 Background - Green

Message - White

MM3-1 thru MM3-4 Background - White

Message - Green

MN3-1 thru MN3-4 Background - Brown

Message - White

6. Note the first letter of each direction is larger than the remainder of the message.

					MN3-4																					
SIZE	Α	В	С	D	E	F	G	Н	I	J K	L	М	N	0	Р	0	R	S	T	U	v	W	Х	Y	Z	Areq sq. ft.
SIZE 1																										
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4 7 1/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8 12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8 12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8 12	12 1/8	14	14 1/8	13			1 1/2									4.5

COUNTY:

STANDARD SIGNS M3-1 thur M3-4 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther & Rauch

For State Traffic Engineer

DATE 6/30/14 PLATE NO. M3-1.13

SHEET NO:

07.001/5...14.675054.4.000000

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\M31.DGN

HWY:

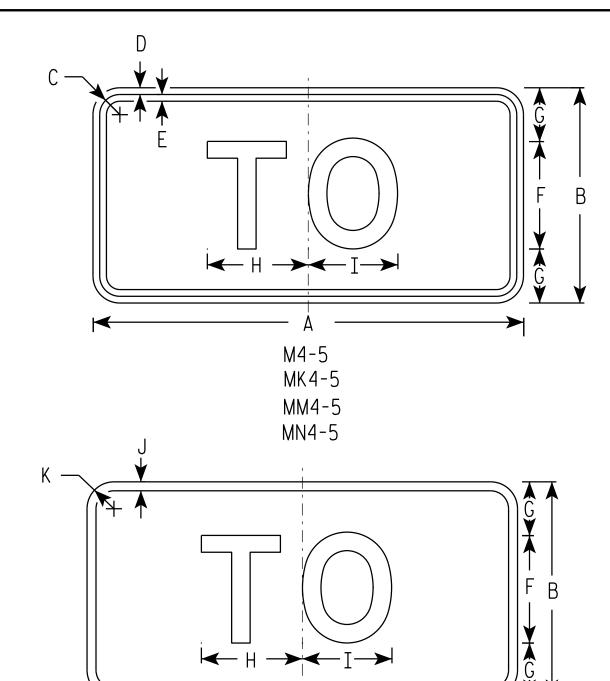
PROJECT NO:

PLOT DATE: 30-JUN-2014 12:53

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 11.675051:1.000000



MB4-5

HWY:

<u>NOTES</u>

- 1. Sign is Type II Type H
- 2. Color:

Background - See note 5 Message - See note 5

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. M4-5 Background White

Message - Black

MB4-5 Background - Blue

Message - White

MK4-5 Background - Green

Message - White

MM4-5 Background - White

Message - Green

MN4-5 Background - Brown

Message - White

SIZE	Α	В	С	D	Е	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	₩	Х	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	5 %	5 1/4	1/2	1 1/2																2.00
3	36	18	1 3/8	3/8	1/2	9	4 1/2	8 1/4	8 %	1/2	1 1/2																4.5
4	36	18	1 3/8	3/8	1/2	9	4 1/2	8 1/4	8 %	1/2	1 1/2																4.5
5	36	18	1 3/8	3/8	1/2	9	4 1/2	8 1/4	8 3/8	1/2	1 1/2																4.5

COUNTY:

STANDARD SIGN M4-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther K Rawl

DATE 6/30/14 PLATE NO. M4-5.7

SHEET NO:

FILE NAME: C:\CAEFiles\Projects\tr_stdplate\M45.DGN

PROJECT NO:

PLOT DATE: 30-JUN-2014 12:48

PLOT NAME :

PLOT BY: mscsja

PLOT SCALE : 5.351066:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series B
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

) A G	
 	; B
→ G →	

Α С E F G H I J S Х Z D 0 10 10 1/4 1 1/8 3/8 3/8 24 2.0 3 36 1 1/8 3/8 1/2 4 1/2 14 5/8 14 1/2 4.5 4 5

COUNTY:

STANDARD SIGN M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 11/10/10 PLATE NO. M4-8.2

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\M48.DGN

PROJECT NO:

HWY:

PLOT DATE: 10-NOV-2010 13:18

PLOT BY : ditjph

PLOT SCALE : 4.767

PLOT NAME :

PLOT SCALE: 4.767233:1.000000

- 1. Signs are Type II See Note 4 reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See note 4 Message - See note 4

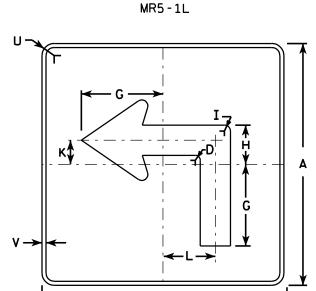
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M5-1 and M5-2 Background - White - Type H Reflective Message - Black
 - MB5-1 and MB5-2 Background Blue Message - White - Type H Reflective
 - MG5-1 and MG5-2 Background Green Message - White - Type H Reflective
 - MK5-1 and MK5-2 Background Green
 - Message White Type H Reflective MM5-1 and MM5-2 Background - White - Type H Reflective
 - Message Green
 - MN5-1 and MN5-2 Background Brown Message - White - Type H Reflective
 - M05-1 and M05-2 Background Orange Type F Reflective
 - Message Black
- MP5-1 and MP5-2 Background White Type H Reflective Message - Blue
- MR5-1 and MR5-2 Background Brown
 - Message Yellow Type H Reflective
- 5. M5-1R same as M5-1L except arrow points right.
- 6. M5-2R same as M5-2L except arrow tilts right.

	c —
	D → E →
←	
M5-2L	

MK5-2L

MM5-2L M05-2L

MP5-2L MR5-2L



MB5-1L

MG5-1L

MN5-1L

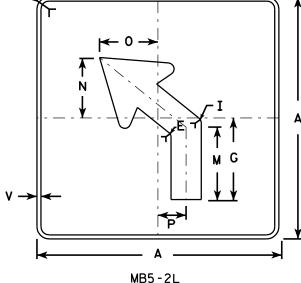
HWY:

M5-1L

MK5-1L

MM5-1L

MO5-1L MP5-1L



MG5-2L

MN5-2L

PLOT BY: mscj9h

1																											
SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	٧	₩	Х	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 %	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 1/8	7 ⁄8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 1/8	1 / ₈		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 %	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 1/8	½		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer PLATE NO. M5-1.12

DATE 7/29/13 SHEET NO:

PLOT NAME :

PLOT DATE: 29-JUL-2013 13:34

PROJECT NO:

- 1. Signs are Type II Type H except as Shown
- 2. Color:

Background - See note 4 Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. M6-1 and M6-2 Background White

Message - Black

MB6-1 and MB6-2 Background - Blue

Message - White

MG6-1 and MG6-2 Background - Green

Message - White

MK6-1 and MK6-2 Background - Green

Message - White

MM6-1 and MM6-2 Background - White

Message - Green

MN6-1 and MN6-2 Background - Brown

Message - White

M06-1 and M06-2 Background - Orange - Type F Reflective

Message - Black

MP6-1 and MP6-2 Background - White

Message - Blue

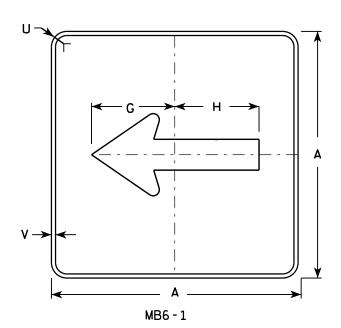
MR6-1 and MR6-2 Background - Brown

Message - Yellow

c —	
D ->	
	A
	M6 - 2
	MK 6 - 2



- MM6-2 MN6 - 2
- MO6-2
- MP6-2
- MR6-2



HWY:

M6 - 1

MK6-1

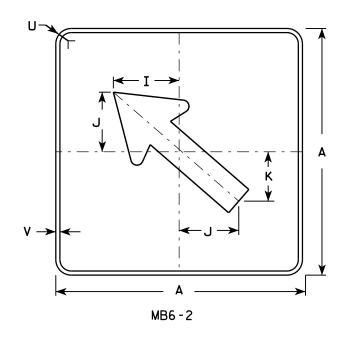
MM6 - 1

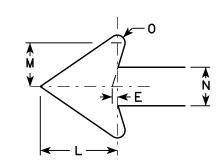
MN6-1

MO6 - 1

MP6-1

MR6-1





SIZE	Α	В	С	D	Е	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 %	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 1/8	1/2					6.25

COUNTY:

STANDARD SIGN M6-1 & M6-2**SERIES**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 7/03/14 PLATE NO. M6-1.14

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\M61.DGN

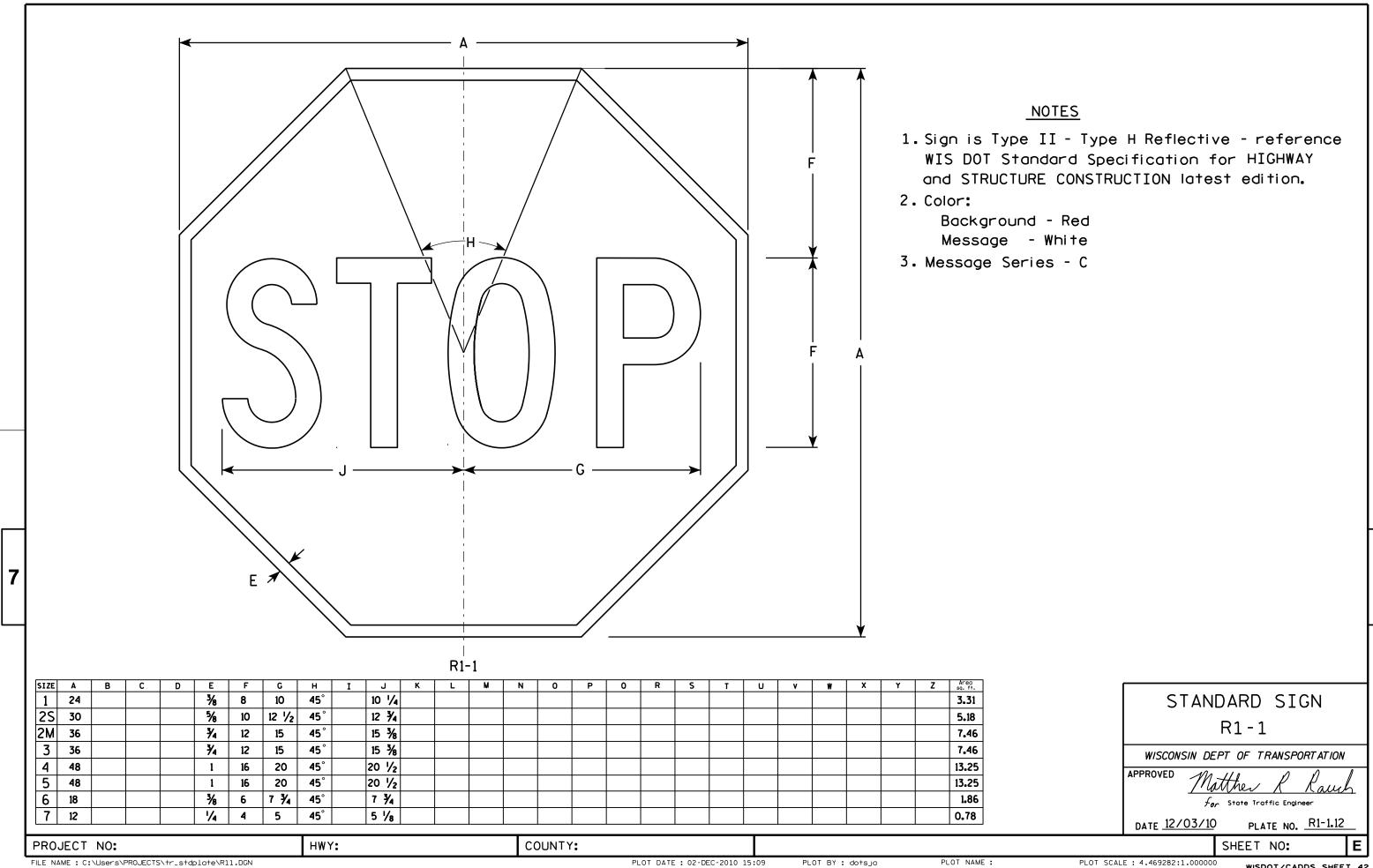
PROJECT NO:

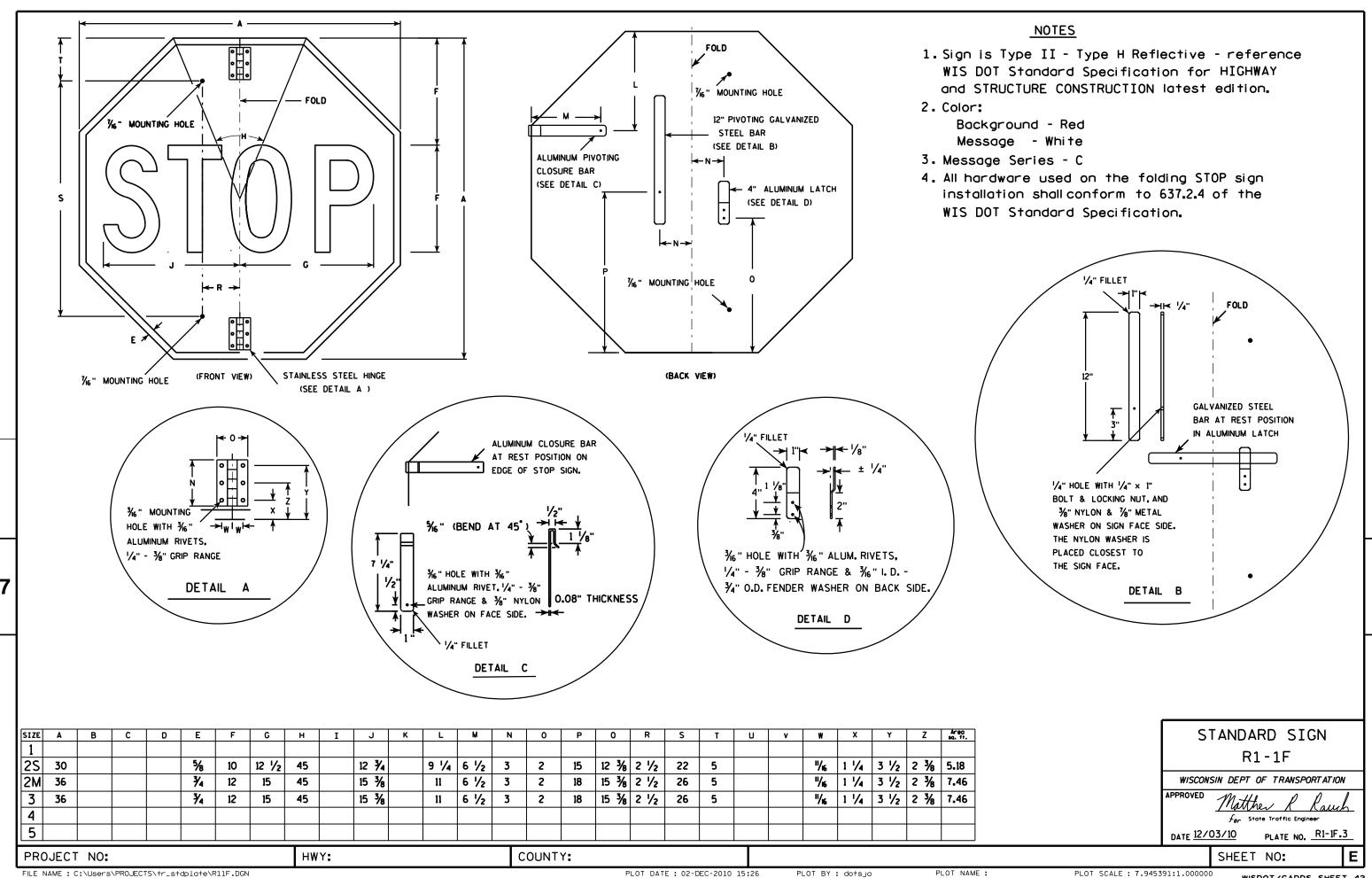
PLOT DATE: 03-JUL-2014 14:28

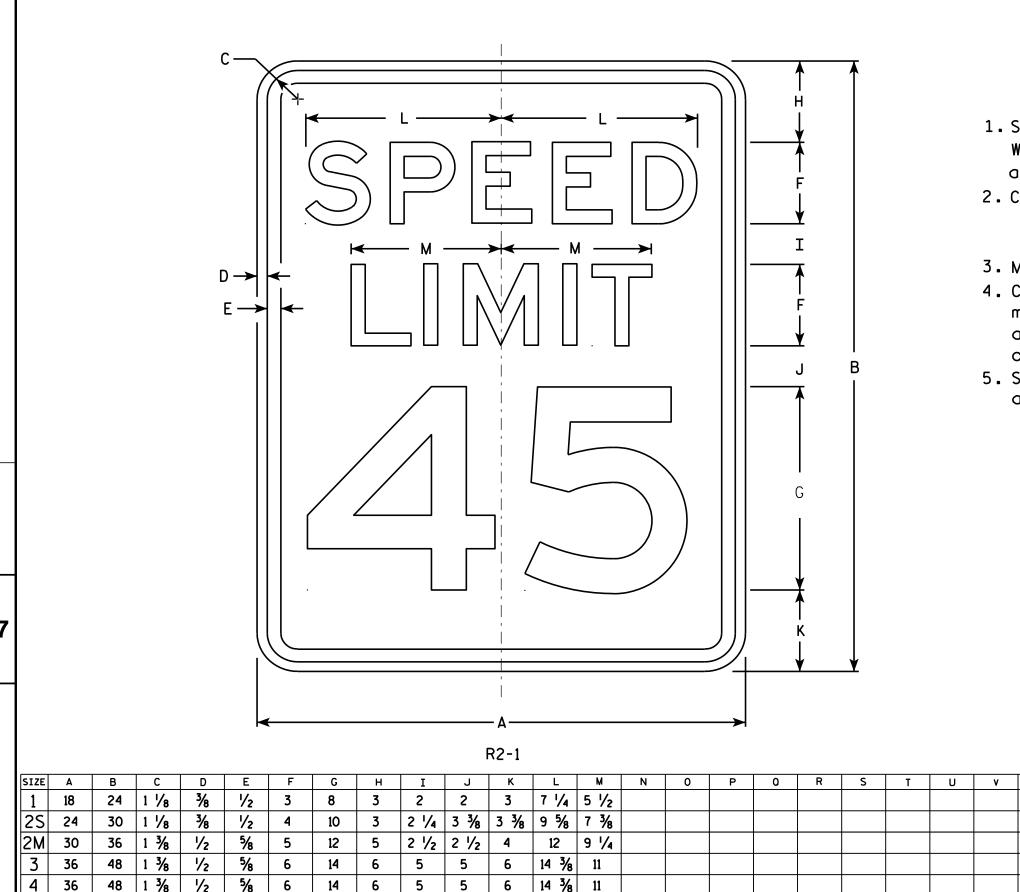
PLOT NAME :

PLOT BY: mscsja

PLOT SCALE: 11.675051:1.000000







4 1/2 6 3/4 6 3/4 19 1/4 14 5/8

COUNTY:

20

HWY:

6

NOTES

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal. the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

3.0

5.0

7.5

12.0

12.0

20.0

STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION APPROVED

Matther R Raus For State Traffic Engineer PLATE NO. R2-1.13

DATE <u>5/26/1</u>0

SHEET NO:

2 1/4

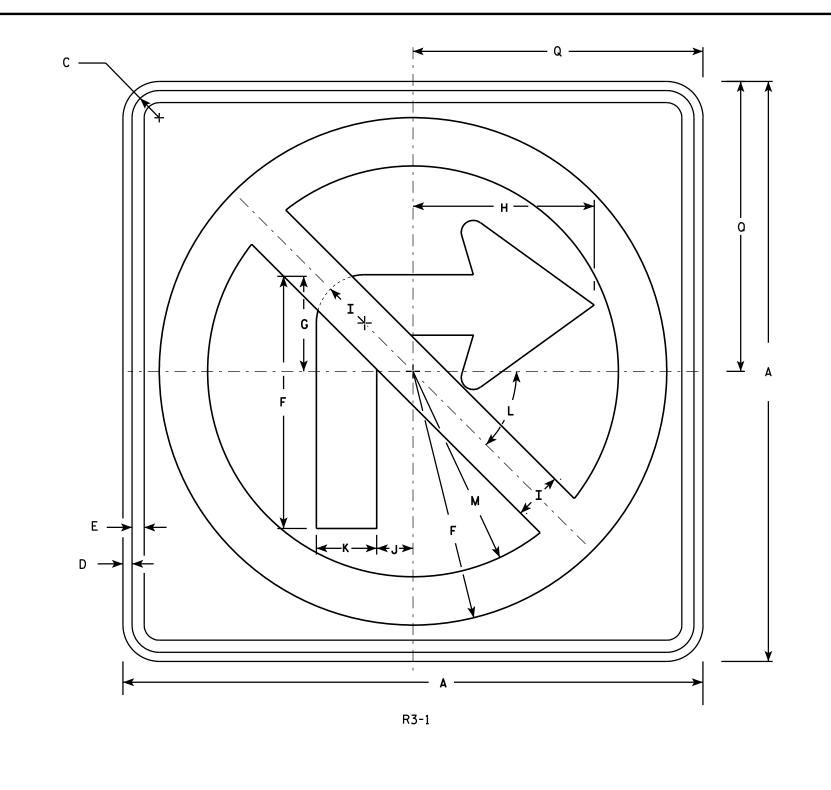
60

5

48

PROJECT NO:

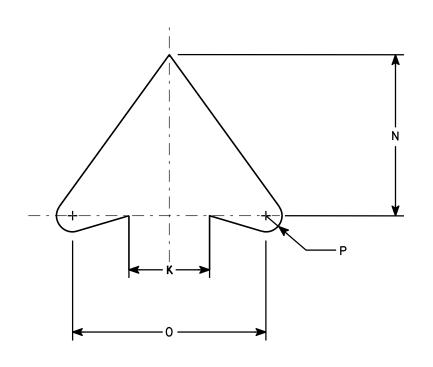
PLOT NAME :



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

PLOT NAME :

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45	8 1/2	5	6	1/2	12										4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	4 5	8 ½	5	6	1/2	12										4.0
2M	36		1 %	5/8	3/4	15 ¾	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
3	36		1 %	5/8	3/4	15 ¾	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
4	36		1 %	5/8	3/4	15 ¾	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4	18										9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0
PRO	PROJECT NO: HWY:												СО	UNTY:													

STANDARD SIGN R3-1

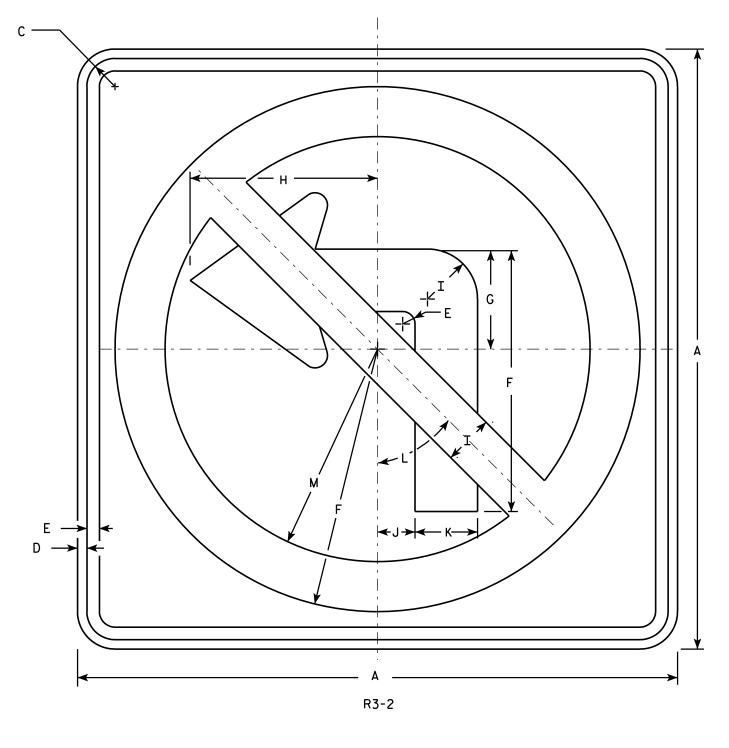
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matther

DATE12/08/10

PLATE NO. __R3-1.5

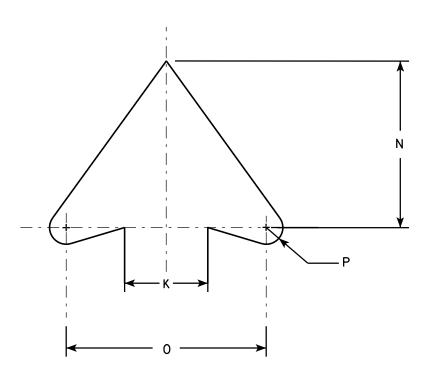
SHEET NO:



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	₩	X	Y	Z	Area sq. ft.
1	24		1 1/8	3⁄8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	9	1/2											4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		1 1/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 ½	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

COUNTY:

STANDARD SIGN R3-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

 $f_{\it or}$ State Traffic Engineer

DATE 12/08/10

PLATE NO. R3-2.10

SHEET NO:

HWY:

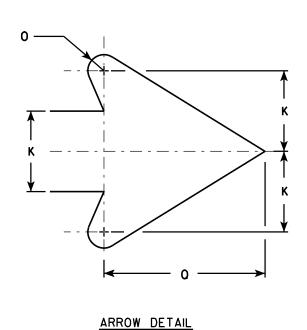
PROJECT NO:

PLOT NAME :

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - See note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



c	* *
	G V A I I I I I I I I I

l																											
SIZE	Α	В	С	D	E	F	G	н	I	J	K	L	M	N	0	P	0	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
25	24		1 1/8	3∕8	1/2		4 3/4	13 1/4	6	2	2 1/2	5 1/4	10 1/2	45°	1/2		5										4.0
2M	36		1 %	5/8	3/4		7 1/8	19 1/8	9	3	3 3/4	7 1/8	15 ¾	45°	3/4		7 5/8										9.0
3	36		1 %	5/8	3/4		7 1/8	19 1/8	9	3	3 3/4	7 1/8	15 ¾	45°	3/4		7 %										9.0
4	36		1 %	5/8	3/4		7 1/8	19 %	9	3	3 3/4	7 1/8	15 ¾	45°	3/4		7 %										9.0
5	36		1 %	5/8	3/4		7 1/8	19 1/8	9	3	3 3/4	7 1/8	15 ¾	45°	3/4		7 %										9.0

COUNTY:

R3-4

STANDARD SIGN R3-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

 $f_{\it or}$ State Traffic Engineer PLATE NO. __R3-4.11

DATE12/08/10 SHEET NO:

PLOT NAME :

PLOT BY: dotsja

PLOT SCALE: 5.959043:1.000000

WISDOT/CADDS SHEET 42

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R34.DGN

HWY:

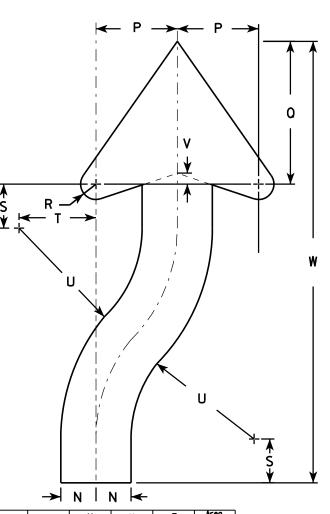
PROJECT NO:

PLOT DATE: 08-DEC-2010 15:34

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
- 2. Color:

Background - White Message - Black

- 3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
- 4. R4-8 is the same as R4-7 except Legend is reversed.



ARROW DETAIL

																							→	N I	N 		
SIZE	Α	В	С	D	Ε	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	Т	U	٧	W	X	Y	Z	Arec sq. f
1	18	24	1 1/8	3∕8	1/2	3 %	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5%	1 %	3 1/4	6 3/4	1/2	20 3/8				3.0
25	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 %	3	8	4	12 1/2	2	30	4 %	8 1/8	1 / ₈	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2N	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 1/8	3	8	4	12 1/2	2	30	4 %	8 1/8	7 ⁄8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 1/8	4 1/2	12	6	18 3/4	3	45	6 %	12 1/4	1 1/4	3 3/4	6 %	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 1/8	4 1/2	12	6	18 3/4	3	45	6 %	12 1/4	1 1/4	3 3/4	6 %	13 1/2	1	40 3/4				12.
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 %	5	8 ¾	18	1 1/4	50 1/4				20.

COUNTY:

R4-7

STANDARD SIGN R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

State Traffic Engineer
3/25/2011 PLATE NO. R4-

DATE 3/25/2011 PLATE NO. R4-7.8

SHEET NO:

PROJECT NO:

D→

HWY:

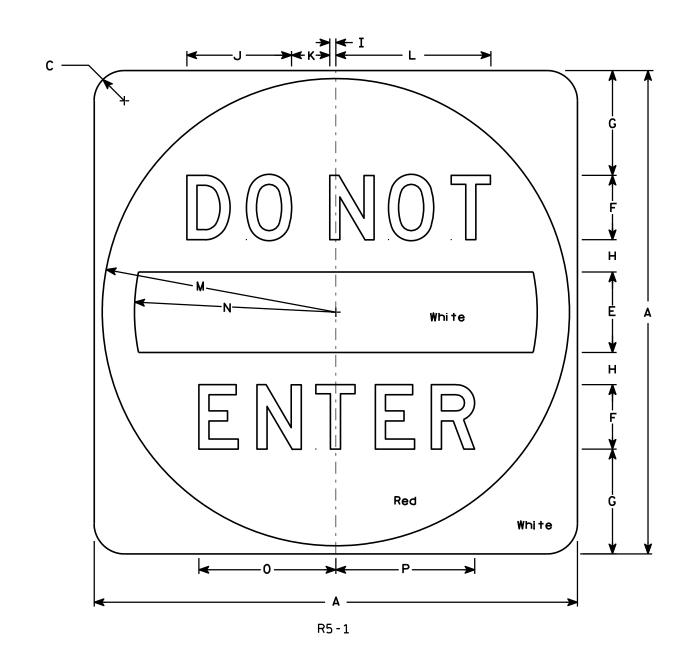
PLOT BY: mscsja

<u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - See detail Message - White - Type H Reflective

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but when base material is metal, the cornors shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	Т	U	V	W	Х	Y	Z	Area sq. ft.
1																											
25	30		1 1/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 %	14 1/2	12 1/2	8 1/2	8 %											6.26
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 ¾											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 %	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 %	14 1/2	23 ½	20	12 3/4	12 1/8											16.0

COUNTY:

STANDARD SIGN R5-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

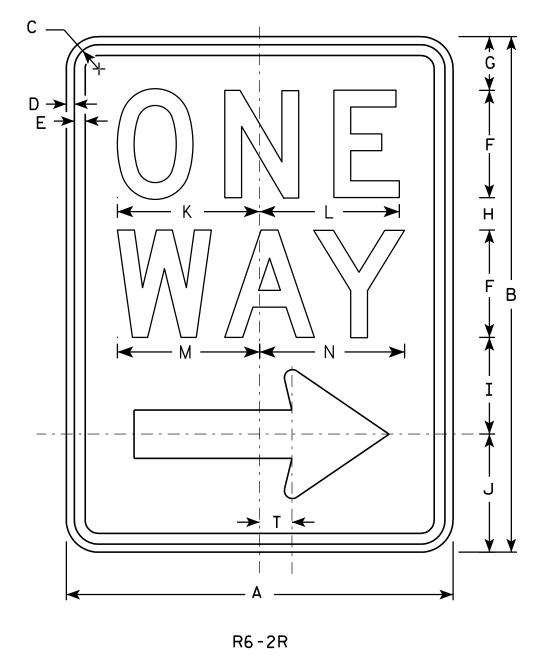
DATE 12/17/10

10 PLATE NO. R5-1.15

Р

PLOT NAME :

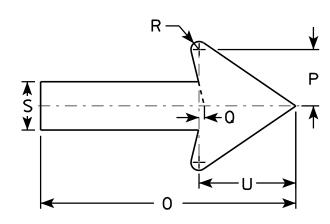
HWY:



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. R6-2L same as R6-2R except arrow points to the left.



SIZE	A	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z
1	18	24	1 1/8	3/8	1/2	5	2 1/2	1 1/2	4 1/2	5 ½	6 %	6 1/2	6 %	6 3/4	11 1/8	2 %	1/4	3/8	2 1/4	1 1/2	4 1/2					
2S	24	30	1 1/8	3/8	1/2	6	3	2 1/2	5 1/2	7	8 1/8	8 1/8	8 1/2	8 %	16	3 1/2	3/8	1/2	3	2	6					
2M	30	36	1 3/8	1/2	5/8	8	2 1/2	2	6 %	8	10 1/2	10 1/2	11 1/4	11 1/4	20	4 3/8	1/2	5/8	3 3/4	2 1/2	7 1/2					
3	36	48	1 1/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 %	1/2	3/4	4 3/4	3	9					
4	36	48	1 1/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 %	1/2	3/4	4 3/4	3	9					
5																										

COUNTY:

STANDARD SIGN R6-2 R&L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

For State Traffic Engineer

DATE 11/2/10

PLATE NO. R6-2.8 SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R62.DGN

HWY:

PROJECT NO:

PLOT DATE: 02-NOV-2010 15:25

PLOT BY: ditjph

PLOT NAME :

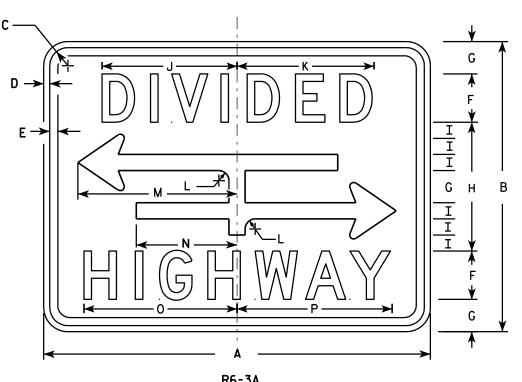
PLOT SCALE: 4.469282:1.000000

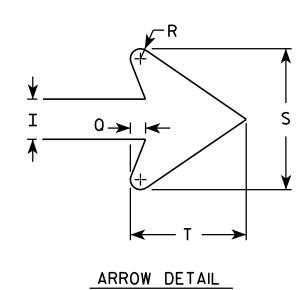
<u>NOTES</u>

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.





		_	
D٤	_	₹	٨

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	24	18	1 1/8	3/8	3/8	3	2	8	1	8 3/8 8	3 1/2	5/8	9 %	6 1/4	9 1/2	9 %	3∕8	1/4	3 1/2	2 3/4							3.0
2S	30	24	1 1/8	3/8	1/2	4	2 5/8	10 ¾	1 3/8	10 1/2 1	10 %	1 / ₈	12 1/2	7 1/8	12 1/4	12 3/8	1/2	3/8	4 %	3 %							5.0
2M	30	24	1 1/8	3/8	1/2	4	2 %	10 ¾	1 3/8	10 1/2 10	10 %	%	12 1/2	7 1/8	12 1/4	12 3/8	1/2	3/8	4 %	3 %							5.0
3																											
4																											
5								·																		·	

STANDARD SIGN R6-3 & R6-3A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

for State Traffic Engineer DATE 3/31/2011 PLATE NO. R6-3.5

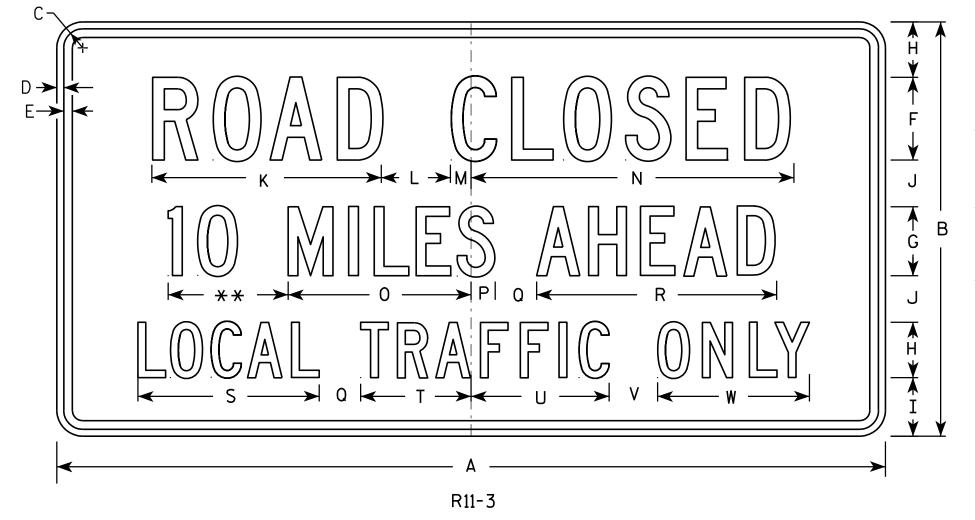
SHEET NO:

5.959043:1.000000

R6-3

PLOT DATE: 31-MAR-2011 09:08

PLOT BY: mscsja



- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

** See Note 5

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	M	Z	0	Р	0	R	S	T	U	v	W	X	Y	Z	Areg
1	36	18	1 3/8	1/2	5/8	4	3	2 1/2	2	2	11 1/8	3	1 1/8	15 1/4	8	1 1/2	2	10 ¾	8 %	4 3/4	6 1/2	2	6 ¾				4.5
2S	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	16 5/8	5	1 1/2	23	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11				12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	16 5/8	5	1 1/2	23	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11				12.5
3																											
4																											
5																											

COUNTY:

STANDARD SIGN R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew & Rauch

DATE 4/1/11 PLATE NO. R11-3.6

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R113.DGN

HWY:

PROJECT NO:

PLOT DATE: 01-APR-2011 14:20

PLOT NAME :

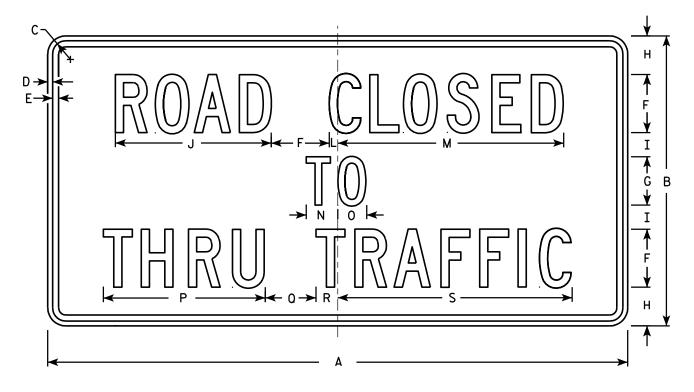
PLOT BY: mscj9h

PLOT SCALE: 6.952216:1.000000

- 1. Sign is Type II Type H Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - White Message - Black

- 3. Message Series C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

SIZE	Α	В	С	D	Ε	F	G	Η	I	J	K	L	М	Z	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7 /8	23 ¾	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7∕8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

COUNTY:

STANDARD SIGN R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

DATE 4/1/11 PLATE NO. R11-4.3

SHEET NO:

PROJECT NO: FILE NAME : C:\Users\PROJECTS\tr_stdplate\R114.DGN HWY:

PLOT DATE: 01-APR-2011 14:11

PLOT BY: mscj9h

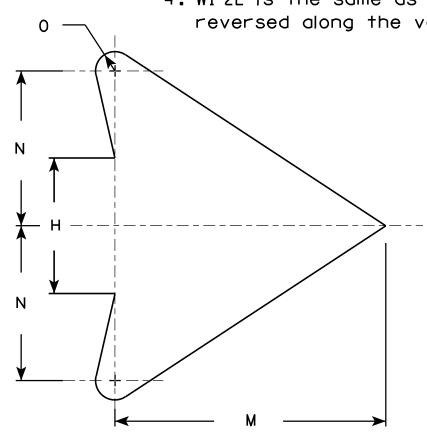
PLOT NAME :

PLOT SCALE: 9.931739:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



								W	1-2R										<u> </u>	11011	DLIA	<u></u>		
SIZE															Area sq. ft.									
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2									4.0
25	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 %	2 1/4	3	9 1/8	8 3/4	5	5/8									6.25
2M	36		1 %	5/8	3/4		12 3/8	5 1/4	6 3/4	2 %	3 1/2	10 1/8	10 1/2	6	3/4									9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 %	3 1/2	10 1/8	10 1/2	6	3/4									9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 %	3 1/2	10 1/8	10 1/2	6	3/4									9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1									16.0
					•	·		•	•								•			•				•

COUNTY:

STANDARD SIGN W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch For State Traffic Engineer

DATE <u>5/15/12</u>

PLATE NO. W1-2.10

SHEET NO:

PROJECT NO:

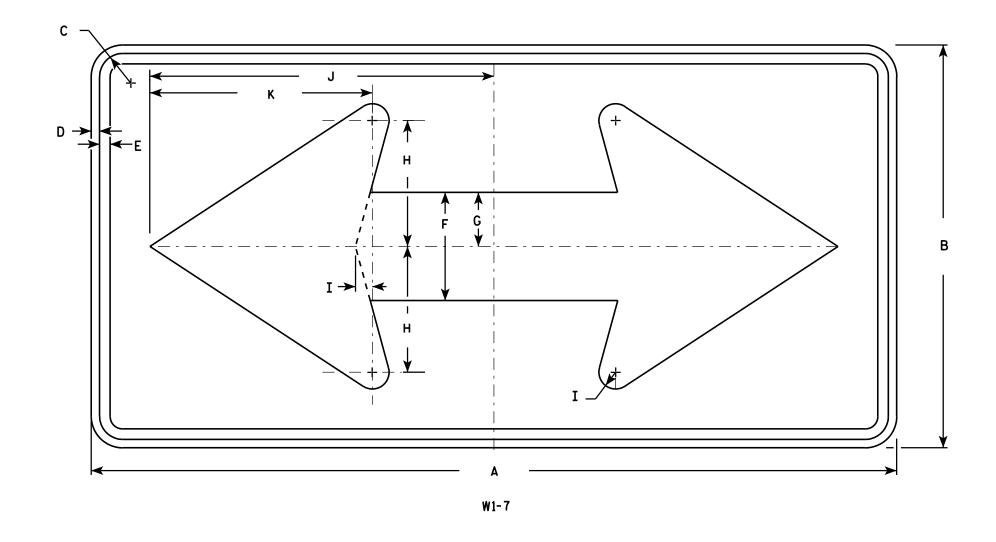
← H →

HWY:

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	₩	Х	Y	Z	Area sq. ft.
1	36	18	1 1/8	3⁄8	1/2	5	2 1/2	5 ¾	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/3	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

COUNTY:

STANDARD SIGN W1-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R

For State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-7.7

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W17.DGN

PROJECT NO:

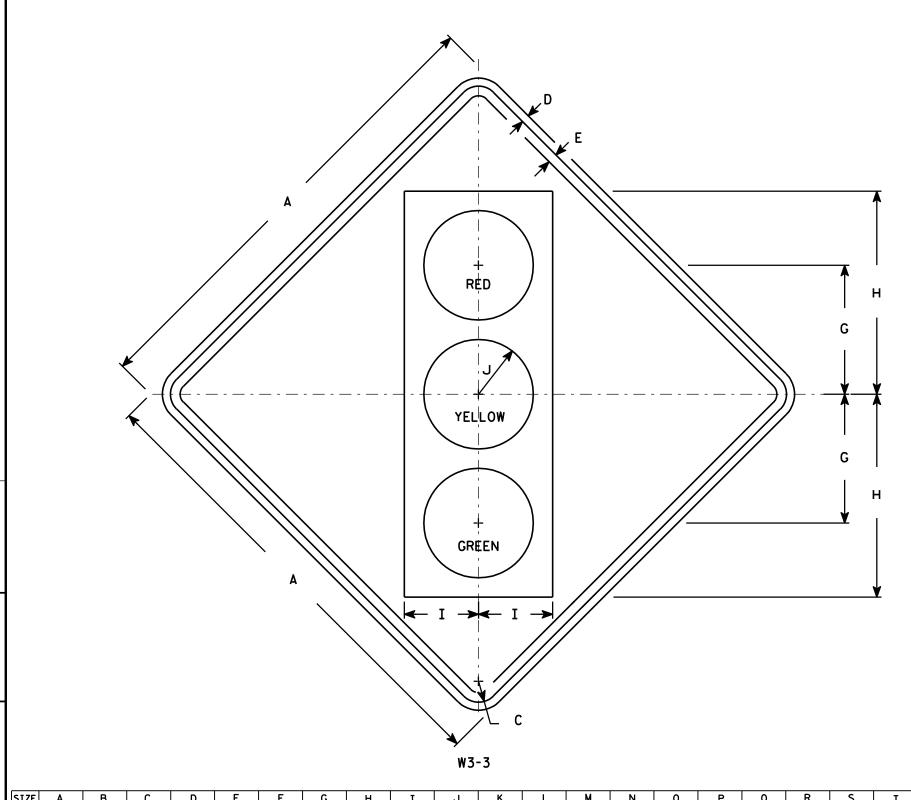
HWY:

PLOT DATE: 07-JUN-2010 12:35

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE: 5.720679:1.000000



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - See Note 4

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. Symbol and border are non-reflective black. Top circle - Type H Reflectorized Red Center circle - Same as background Bottom circle - Type H Reflectorized Green

SIZE Α 1 3/8 1/2 13 3/4 5 5/8 8 3/4 3 3/4 30 6.25 25 1 % 5/8 15 3/4 5 3/4 4 1/4 36 3/4 9.0 2M 15 3/4 5 3/4 4 1/4 36 1 % 5/8 9.0 3 36 1 % 5/8 15 3/4 5 3/4 4 1/4 9.0 3/4 4 12 1/2 20 7 1/2 5 48 2 1/4 16.0 12 1/2 5 20 7 1/2 5 48 2 1/4 16.0

COUNTY:

STANDARD SIGN W3-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED _____

DATE 6/7/10 PLATE NO. W3-3.11

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W33.DGN

HWY:

PROJECT NO:

PLOT DATE: 07-JUN-2010 13:07

PLOT BY : ditjph

PLOT NAME: PLOT S

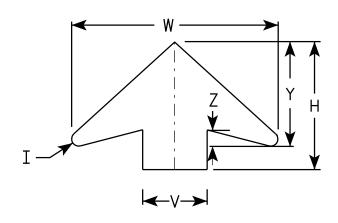
PLOT SCALE: 7.448805:1.000000

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color: *

 Background YELLOW*

 Message BLACK
- 3. Message Series C for numbers Series E for wording
- 4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background



ARROW DETAIL

SIZE	A	В	С	D	E	F	G	н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	×	Y	Z	Area sq. ft
1																											
25	36		1 %	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 %	9.0
2M	36		1 %	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 %	9.0
3	36		1 %	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3∕8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 %	9.0
4	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	1 /8	30	2 1/4	4	1 1/4	15	10	1 %	1/2	8	9 1/4	9 3/8	12	8	25 %	3/8	13	2	16.0
5	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7 /8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 %	3/8	13	2	16.0

STANDARD SIGN W3-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch.

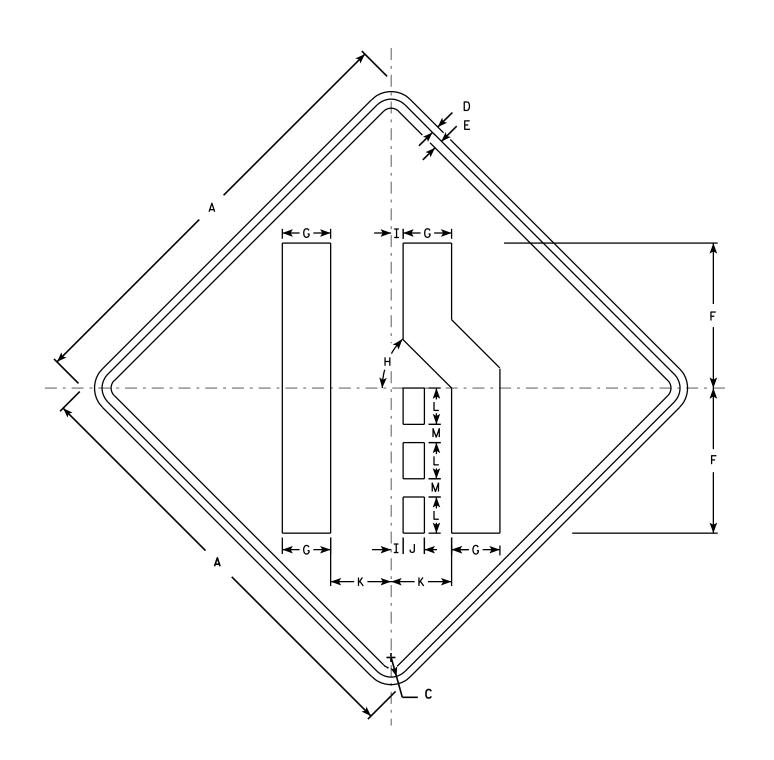
DATE 5/29/12 PLATE NO. W3-5.5

SHEET NO:

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W4-2L is the same as W4-2R except the symbolis reversed along the vertical centerline.



W4-2R

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	M	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	10	3 3/8	45°	1 / ₈	1 1/2	4 1/4	2 1/2	1 1/4														6.25
25	36		1 1/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2M	36		1 1/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
3	36		1 1/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
4	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		2 1/4	3/4	1	16	5 %	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
	JECT	NO:	- /4				1 78		- /4	- 78	- /4		<u> </u>			<u> </u>							<u> </u>				_

STANDARD SIGN W4-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

DATE 3/12/13

PLATE NO. W4-2.14 SHEET NO:

For State Traffic Engineer

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W42.DGN

PLOT DATE: 12-MAR-2013 11:09

PLOT BY: mscsja

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow

3. Corners may be square or rounded when base material is plywood. When base material is metal the corners shall be rounded.

	>
W5-54	

SIZE	Α	В	С	D	E	F	G	Н	I	C	K	L	М	N	0	Р	a	R	S	Т	U	٧	W	X	Υ	Z	Area sq. ft.
1	12								1																		1.0
2S	18								1 1/2																		2.25
2M	18								1 1/2																		2.25
3																											
4																											
5																							·				
													1														

COUNTY:

STANDARD SIGN W5-54

WISCONSIN DEPT OF TRANSPORTATION APPROVED

Matthew R Rauch DATE 11/3/10 PLATE NO. W5-54.8

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W554.DGN

PROJECT NO:

HWY:

PLOT DATE: 03-NOV-2010 09:54

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE: 4.965871:1.000000

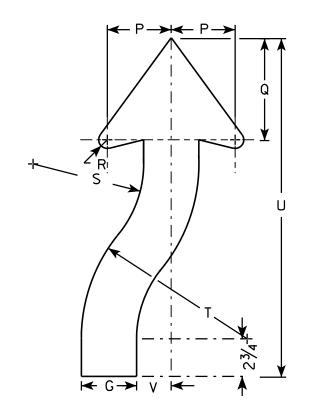
WISDOT/CADDS SHEET 42

Ε

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W6-2 same as W6-1 but is rotated 180° when mounted.



ARROW DETAIL

PLOT NAME :

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	M	N	0	Ρ	0	R	S	T	U	٧	₩	Х	Y	Z	Areg sq. ft.
1	30		1 3/8	1/2	5/8		3 1/4	8	8 1/4	4 1/8	7 1/8	25	1 3/4	11 %	4 1/8	3 %	6 3/4	5/8	6 %	9 %	21 %	2					6.25
2S	36		1 1/8	5/8	3/4		4	8 3/4	10	4 3/4	9 1/2	30	2	14	5	4 %	7 3/8	7 /8	8	12	24 1/2	2 1/2					9.0
2M	36		1 1/8	5/8	3/4		4	8 3/4	10	4 3/4	9 1/2	30	2	14	5	4 %	7 3/8	7 /8	8	12	24 1/2	2 1/2					9.0
3																											
4	48		2 1/4	3/4	1		5 3/8	11 %	13 3/8	6 3/8	12 5/8	40	2 5/8	18 %	6 5%	6 1/4	9 %	1 1/4	10 %	16	32 %	3 3/8					16.0
5	48		2 1/4	3/4	1		5 3/8	11 5/8	13 3/8	6 3/8	12 5/8	40	2 %	18 %	6 %	6 1/4	9 %	1 1/4	10 %	16	32 %	3 3/8					16.0

COUNTY:

W6-1

← G → ← G →

STANDARD SIGN W6-1 & W6-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther R Rauch
For State Traffic Engineer

DATE <u>03/12/13</u>

PLATE NO. W6-1.14

SHEET NO:

PROJECT NO:

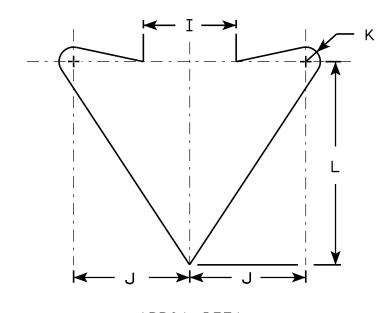
HWY:

<u>NOTES</u>

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	Α	В	С	D	Ε	F	G	Н	I	J	К	L	М	N	0	Р	0	R	S	Т	U	٧	W	Х	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	10 1/2	1	3 %	4	5	5/8	8 3/4	5 %														6.25
2S	36		1 %	5/8	3/4	12	1	4 1/4	5	6	3/4	10 1/2	6 3/4														9.0
2M	36		1 1/8	5/8	3/4	12	1	4 1/4	5	6	3/4	10 1/2	6 3/4														9.0
3																											
4	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0
5	48		2 1/4	3/4	1	15 1/2	1	6	6	8	1	14	9														16.0

COUNTY:

W6-3

HWY:

STANDARD SIGN W6-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED _/

Matthe R Rauch

For State Traffic Engineer

DATE 03/12/13 PLATE NO. W6-3.10

SHEET NO:

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W63.DGN

PROJECT NO:

PLOT DATE: 12-MAR-2013 14:11

PLOT BY: mscsja

PLOT SCALE: 6.202372:1.000000



- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Message Series D
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. W9-1L same as W9-1R except the word Left replaces Right.

	I -			J —							-1R						G A		
+	C 1 3/	D I/-	E 5/	F	G	9 1/-	I	J - 5/	K	L L	M - 5/	N 5/	0	Р	0	R	S	T	U

SIZE	Α	В	С	D	E	F	G	Н	I	J	К	L	M	N	0	Р	0	R	S	Т	U	v	W	Х	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	5	1 1/2	2 1/2	7 1/8	7 %	9 1/4	8 1/8	7 %	8 %													6.25
2S	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
2M	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
3	36		1 5/8	5/8	3/4	6	2	3	8 1/2	9 1/8	11	9 3/4	9	10 3/8													9.0
4	36		1 5/8	5/8	3/4	6	2	3	8 ½	9 1/8	11	9 3/4	9	10 3/8													9.0
5	48		2 1/4	3/4	1	8	3	4	11 1/4	12 1/4	14 3/4	12 1/8	12 1/4	13 %													16.0
PRO.	JECT	NO:																									

STANDARD SIGN W9-1

WISCONSIN DEPT OF TRANSPORTATION

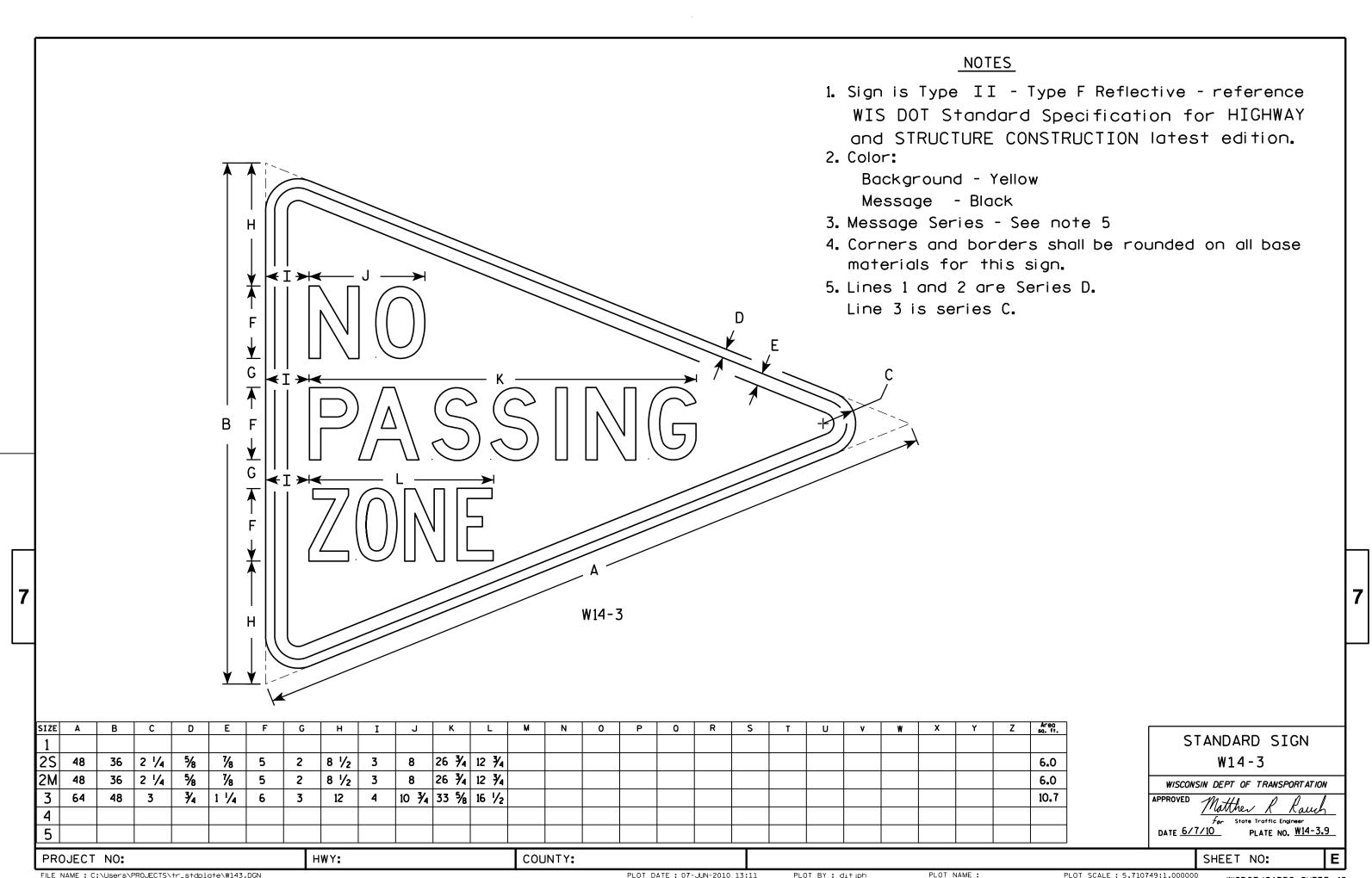
APPROVED

For State Traffic Engineer

DATE 03/18/13

PLATE NO. W9-1.8

SHEET NO:

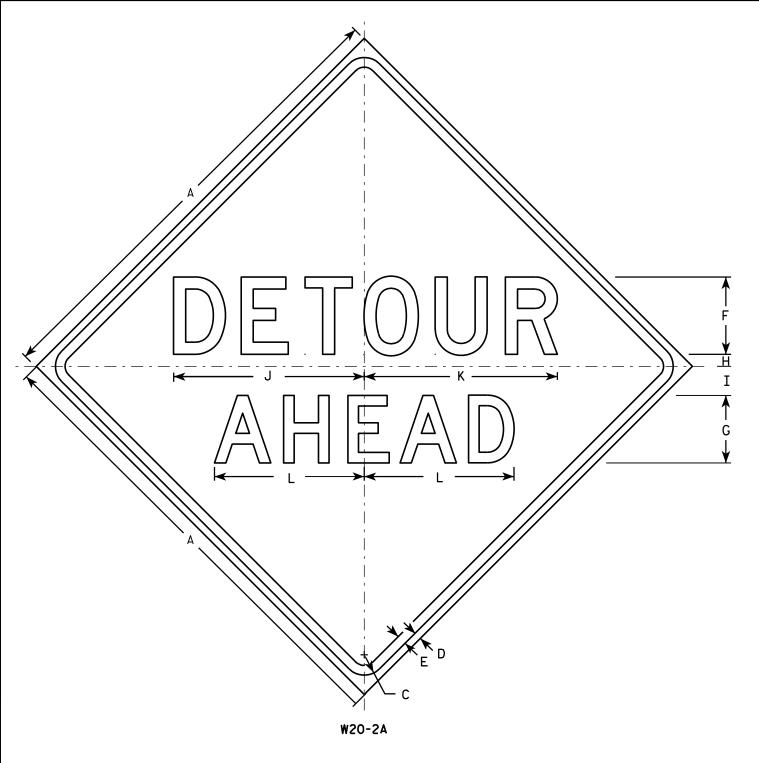


FILE NAME : C:\Users\PROJECTS\tr_stdplate\W143.DGN

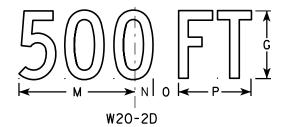
PLOT DATE: 07-JUN-2010 13:11

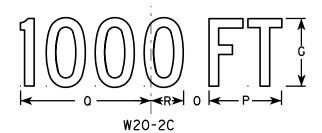
PLOT BY: ditjph

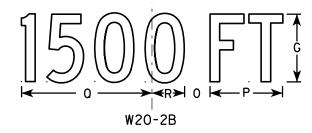
PLOT SCALE: 5.710749:1.000000

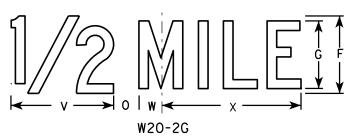


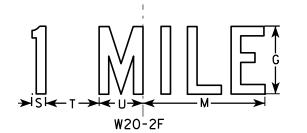
HWY:











PLOT BY: mscj9h

NOTES

- Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Orange Message - Black

- 3. Message Series See note 5
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Line 1 is Series D.
 Line 2 is Series D for AHEAD and
 Series C for all other distances.

SIZE	. Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	36		1 1/8	5/8	₹4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 1/8	5 %	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
3	48		2 1/4	₹4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 %	7 1/2	13 ½	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
4	48		2 1/4	₹4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 1/8	2 %	7 1/2	13 1/2	3 %	1 1/2	6	4 %	10 %	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 ¾	20	15 1/2	12	1 1/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 %	2 3/8	14 3/8	·		16.0

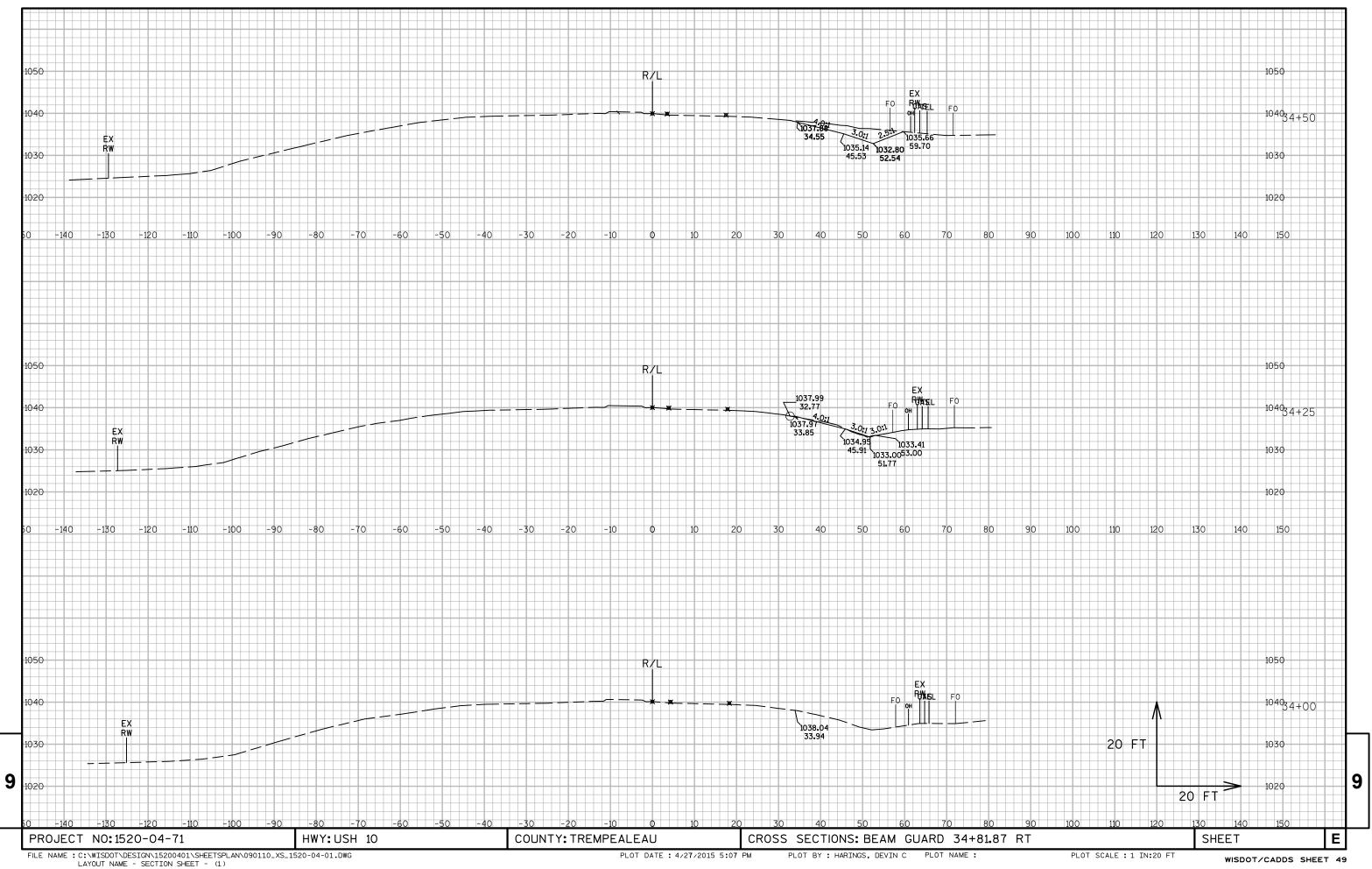
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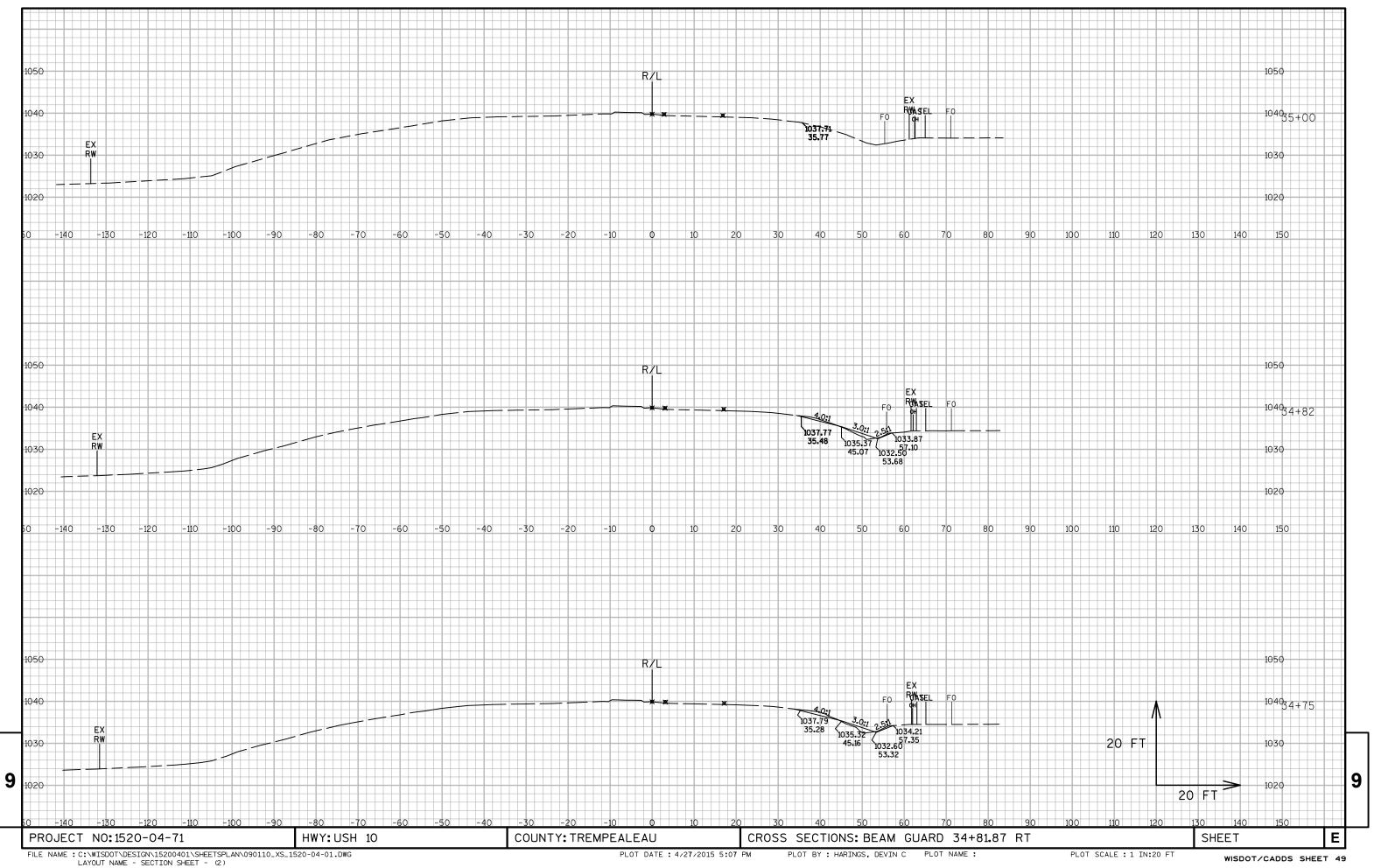
STANDARD SIGN W20-2A,B,C,D,F & G

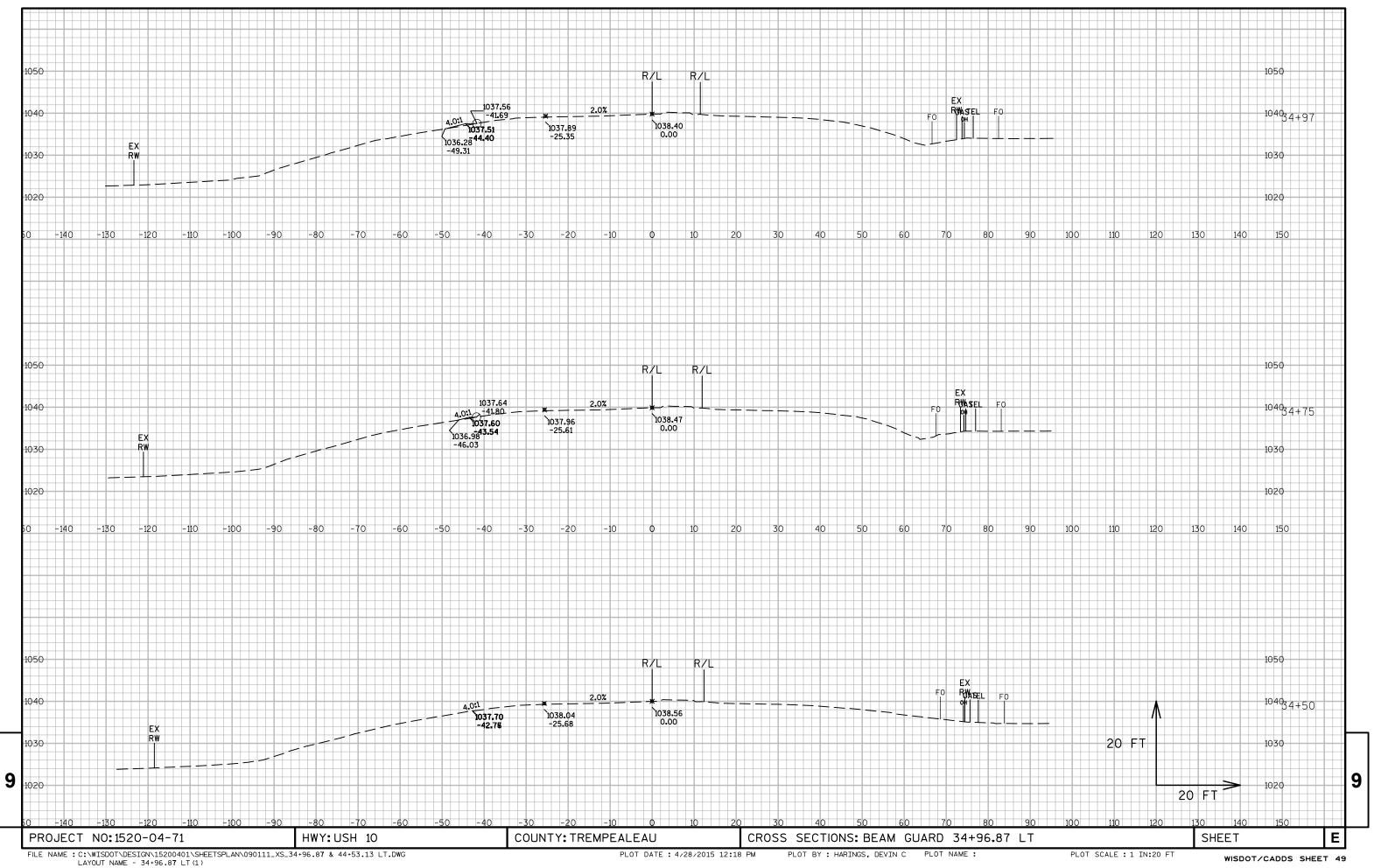
WISCONSIN DEPT OF TRANSPORTATION

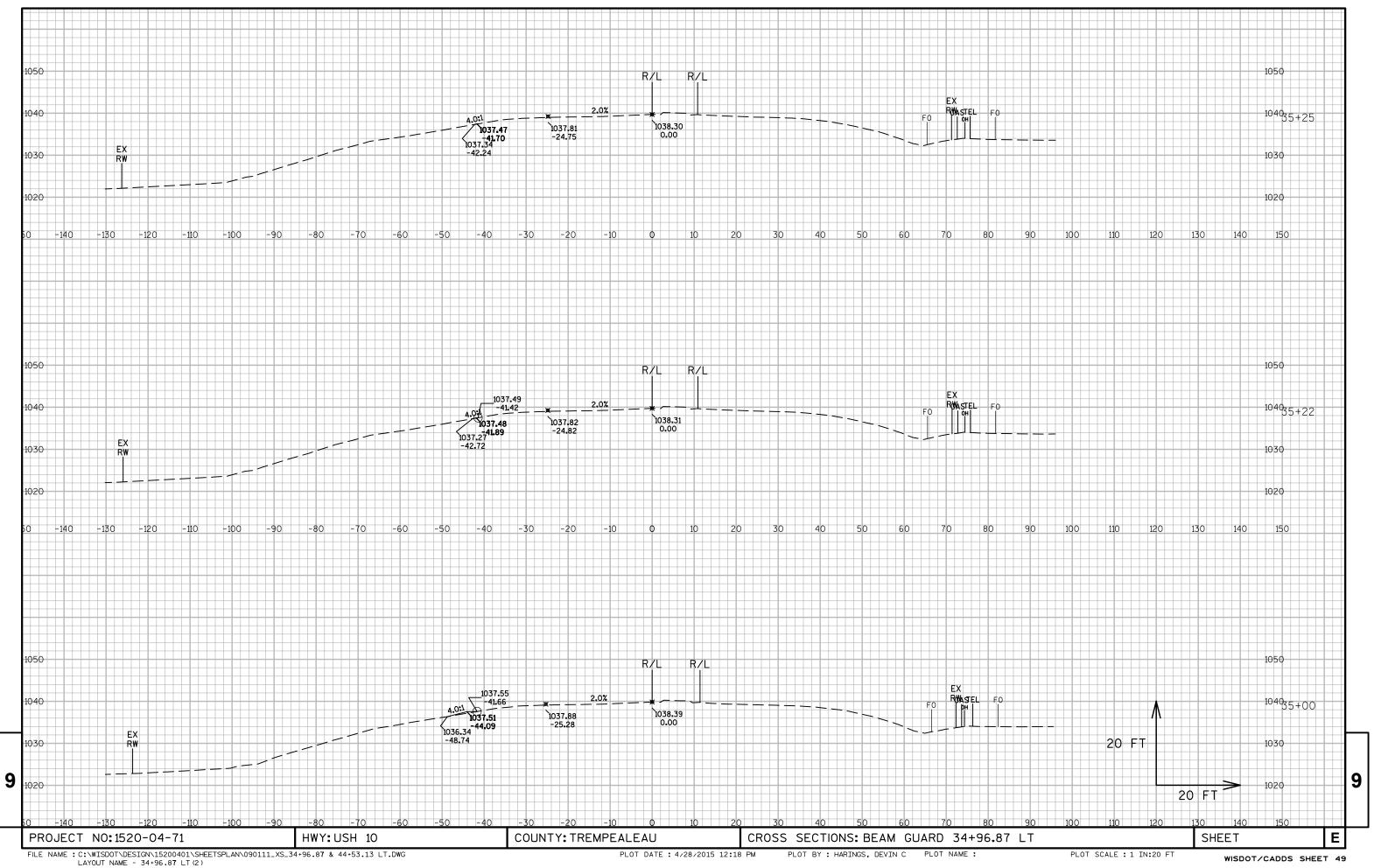
DATE 3/18/11 PLATE NO. W20-2.6

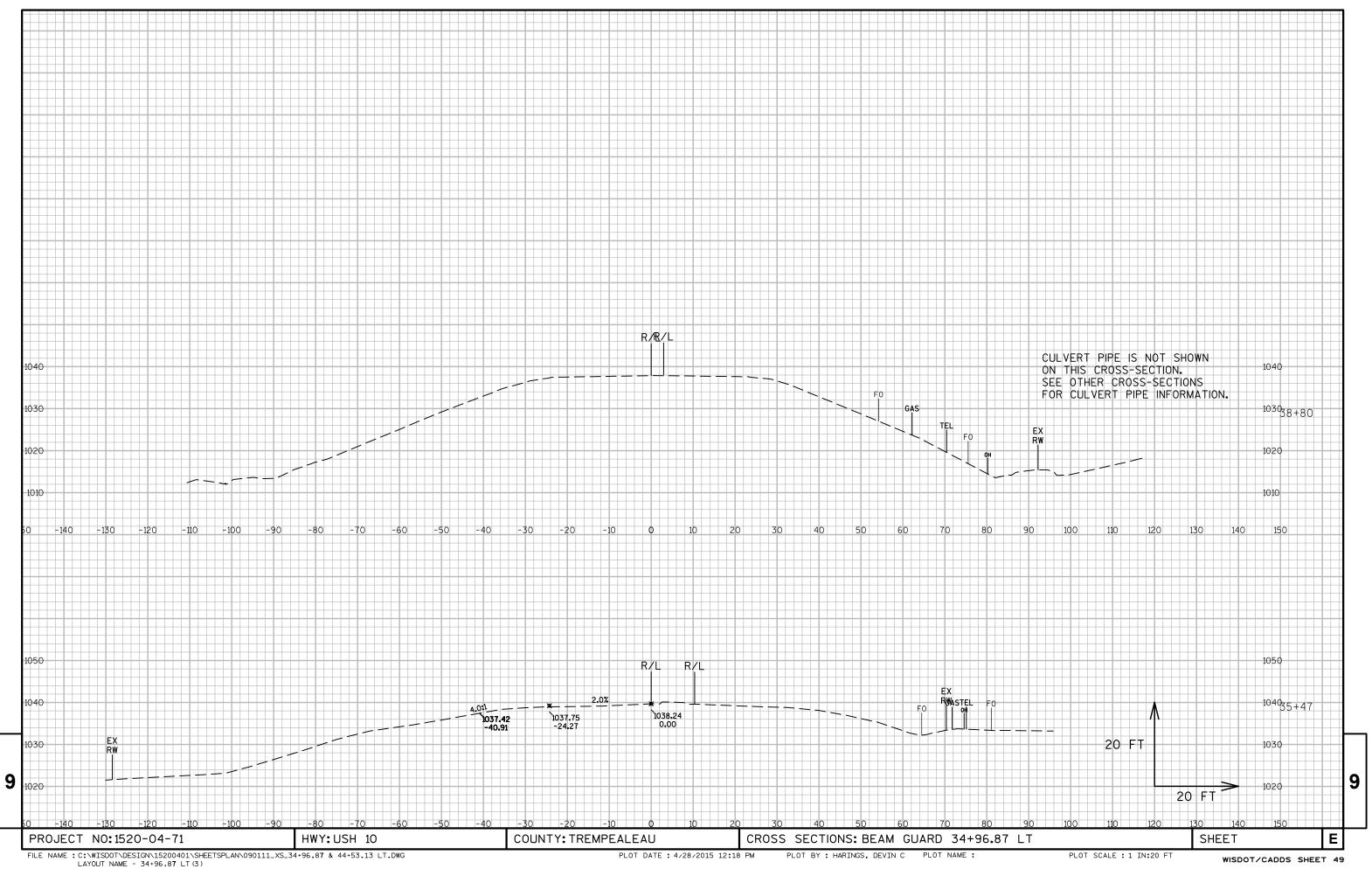
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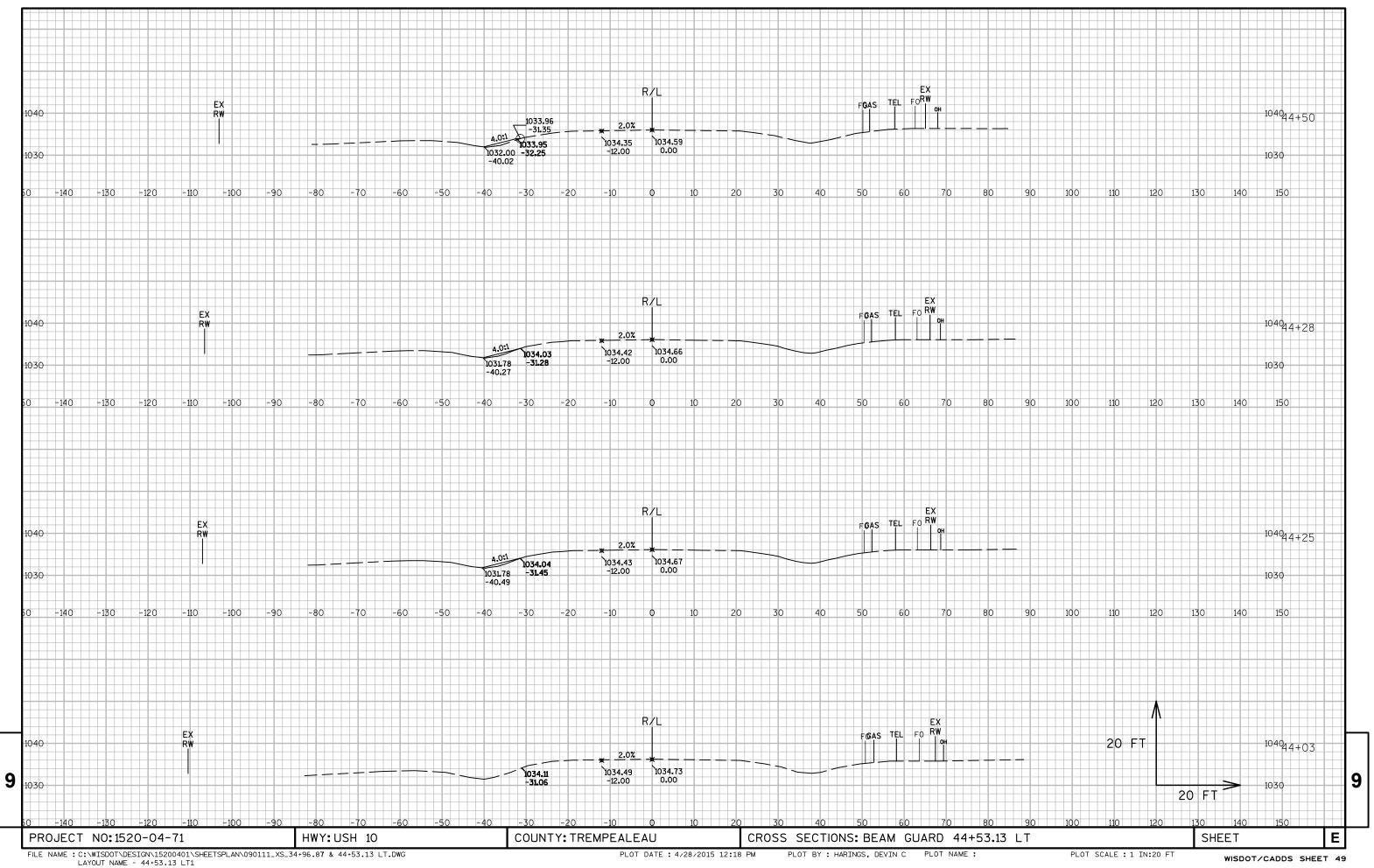


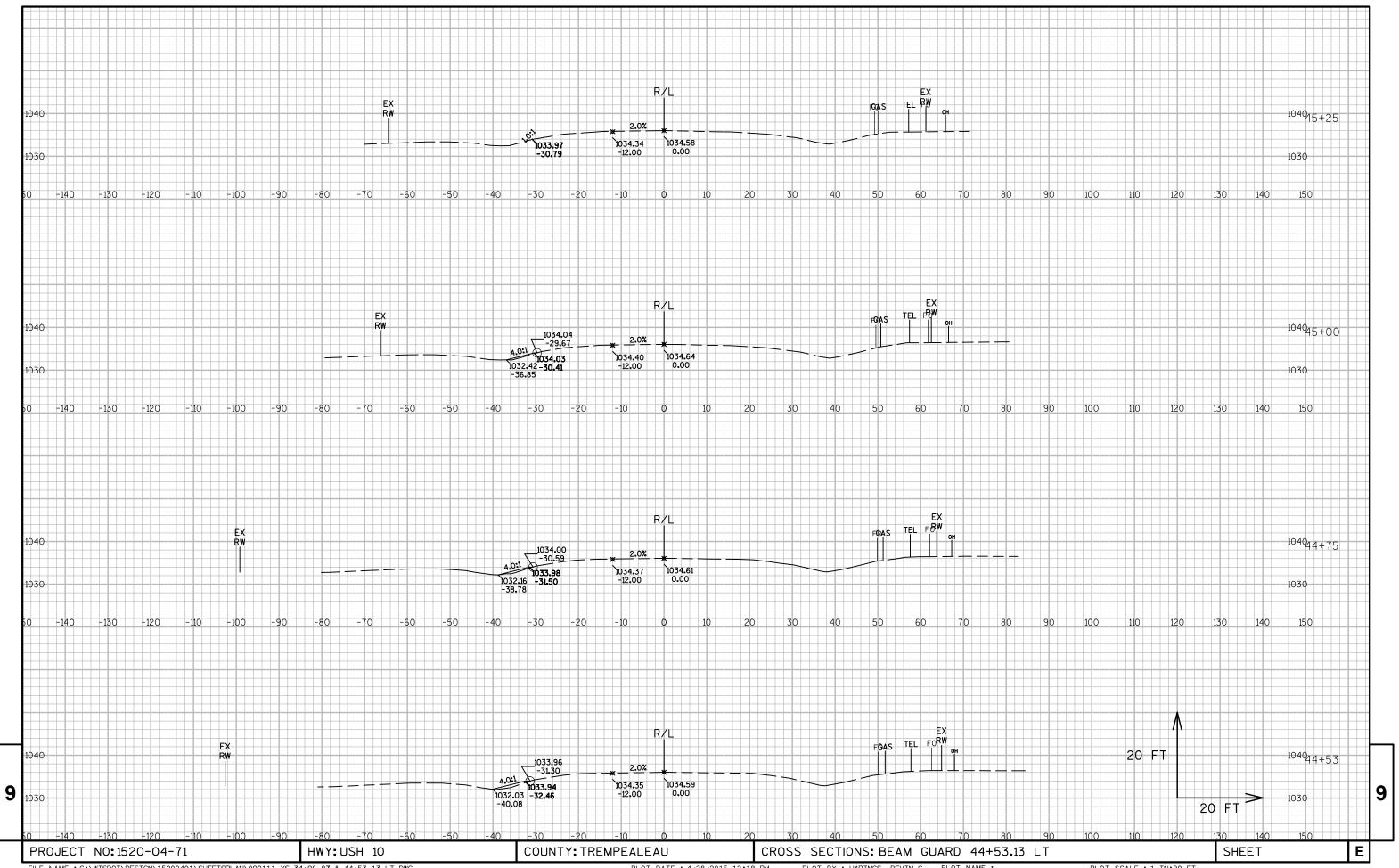


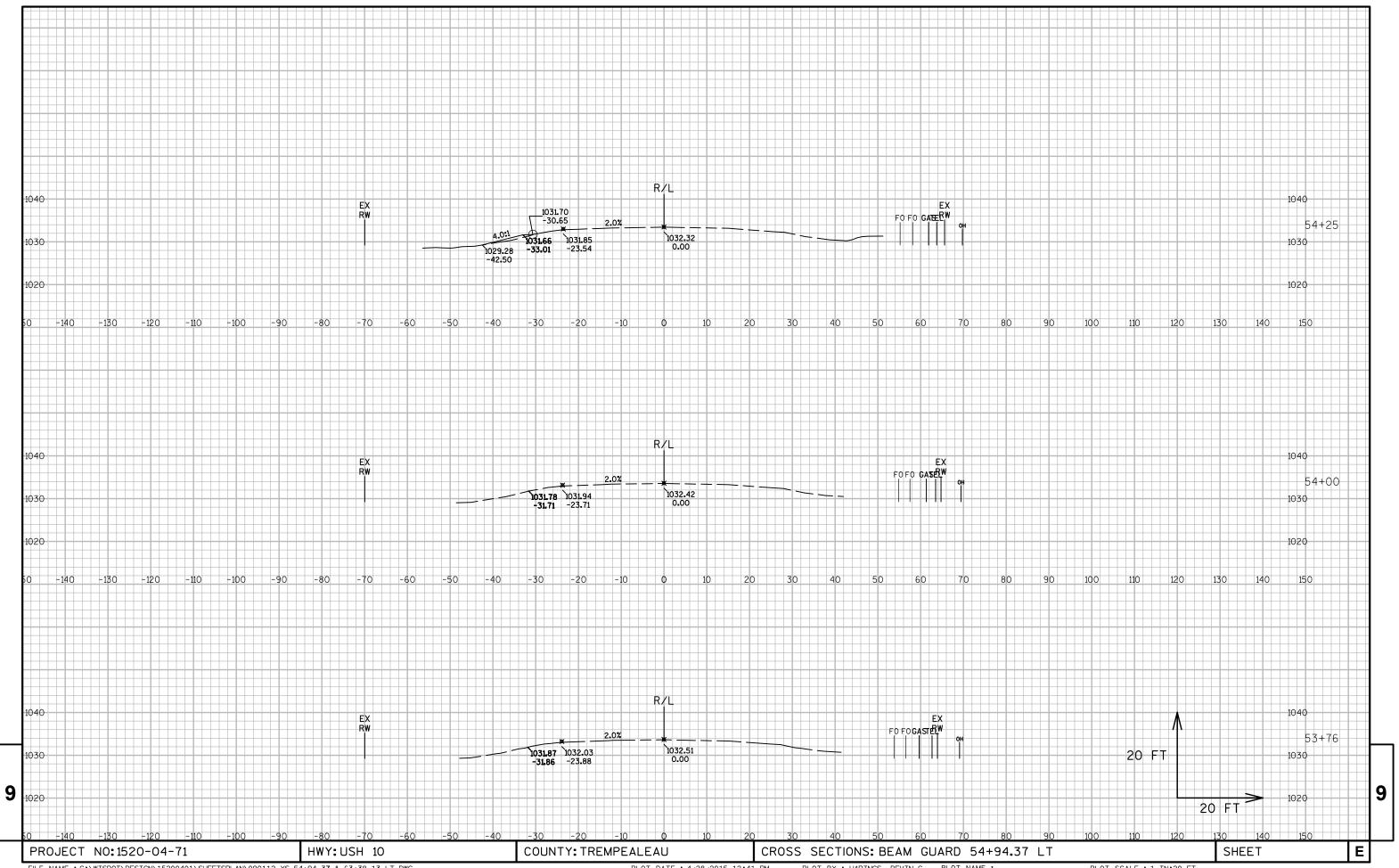


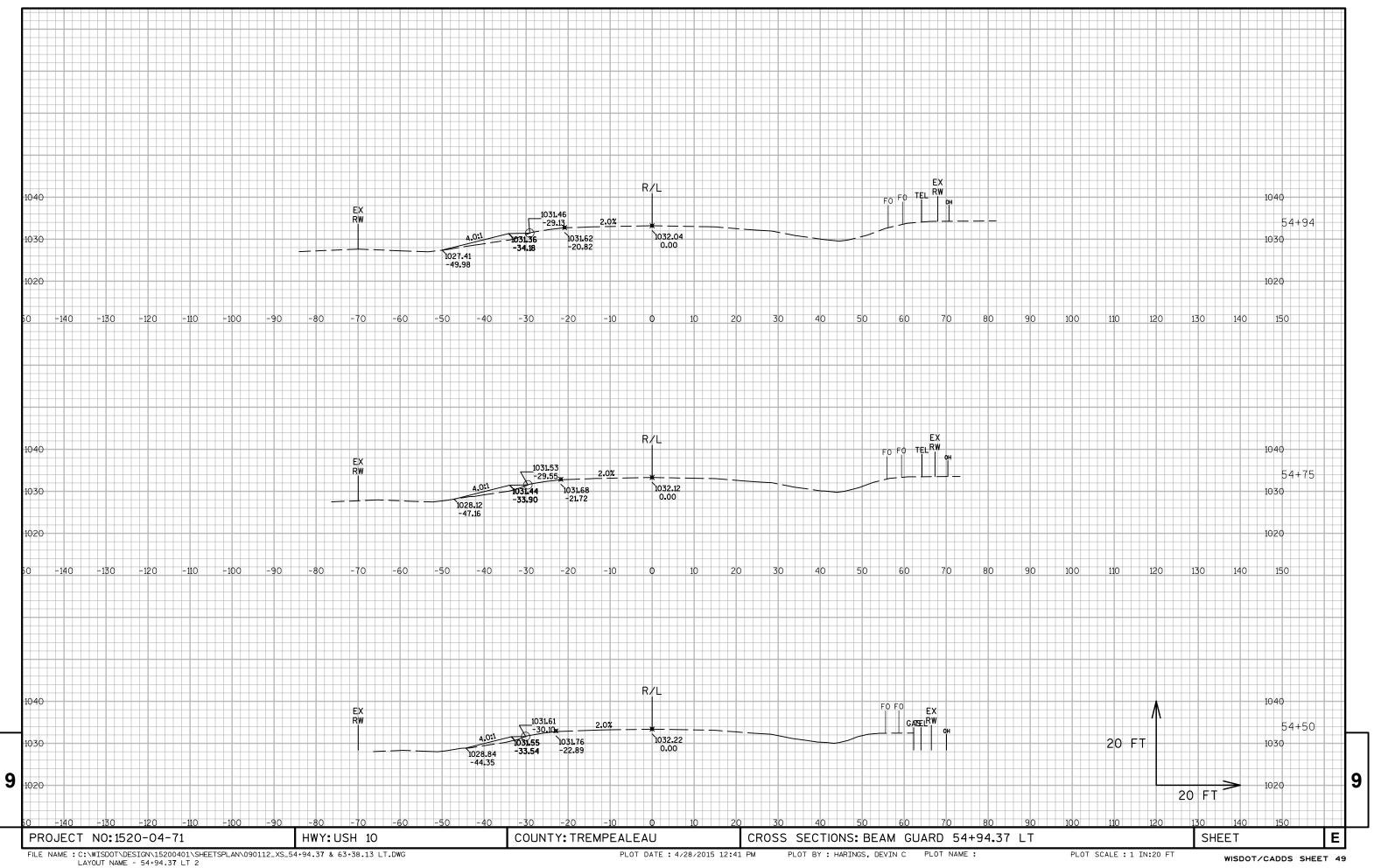


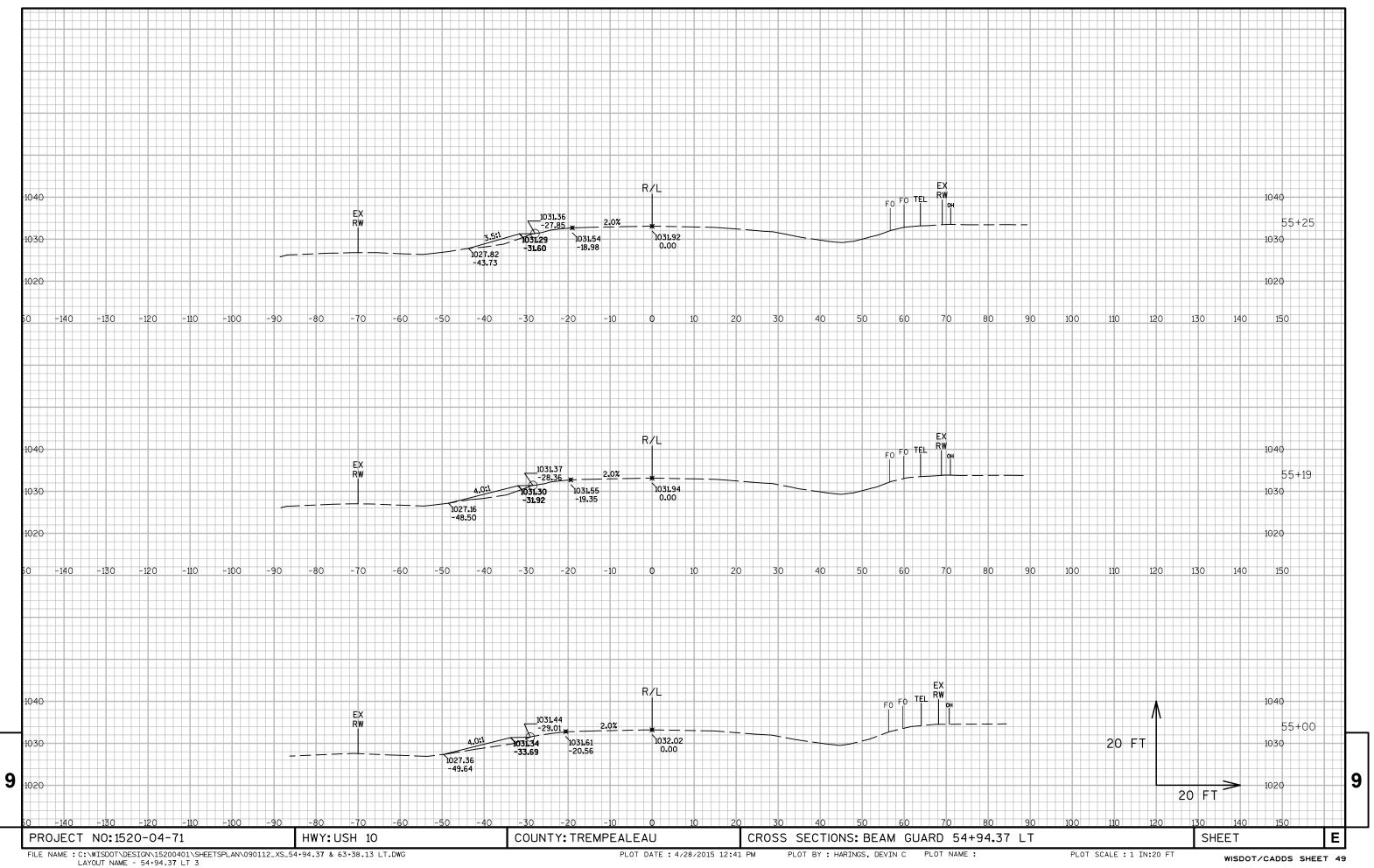




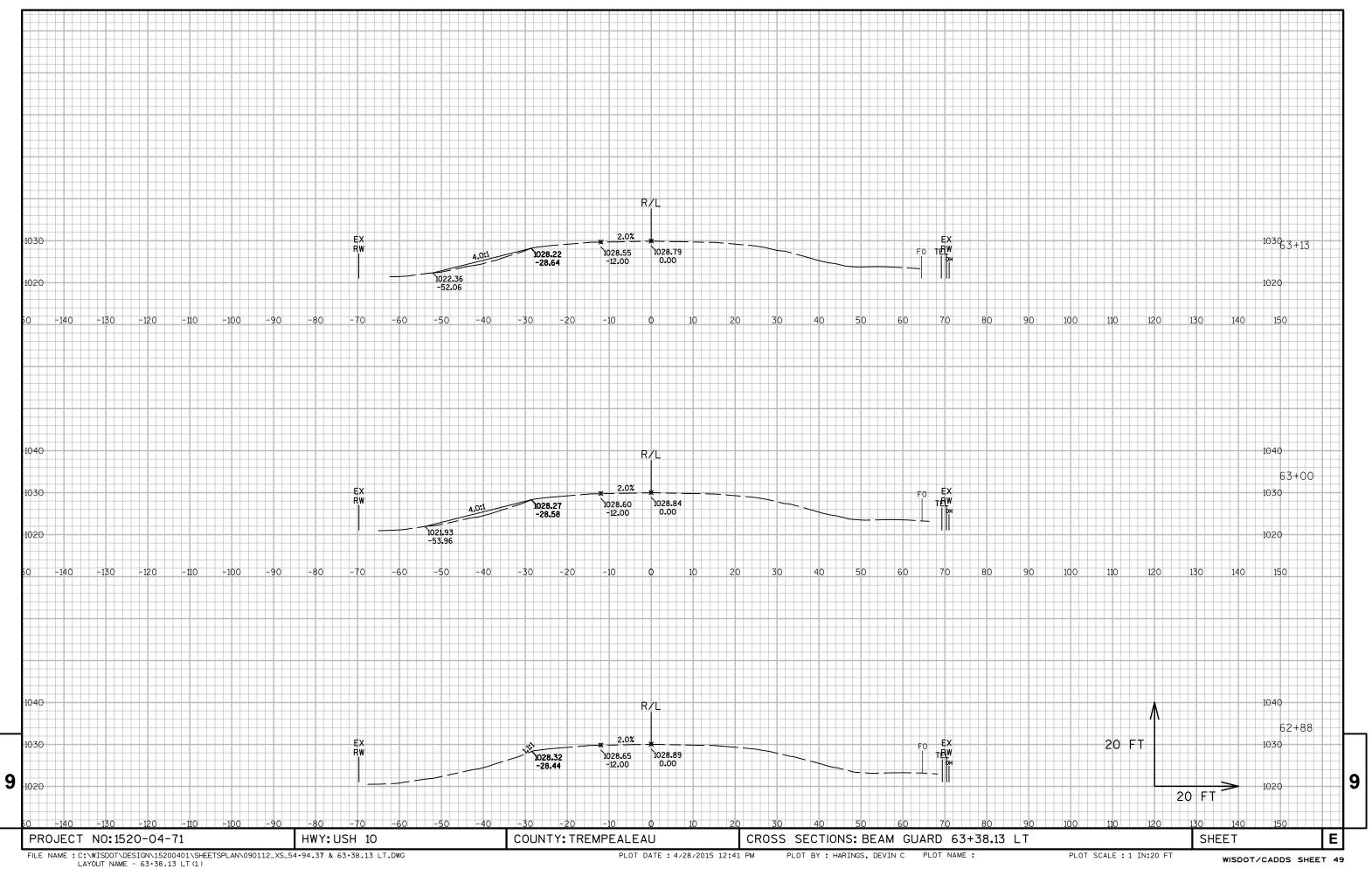


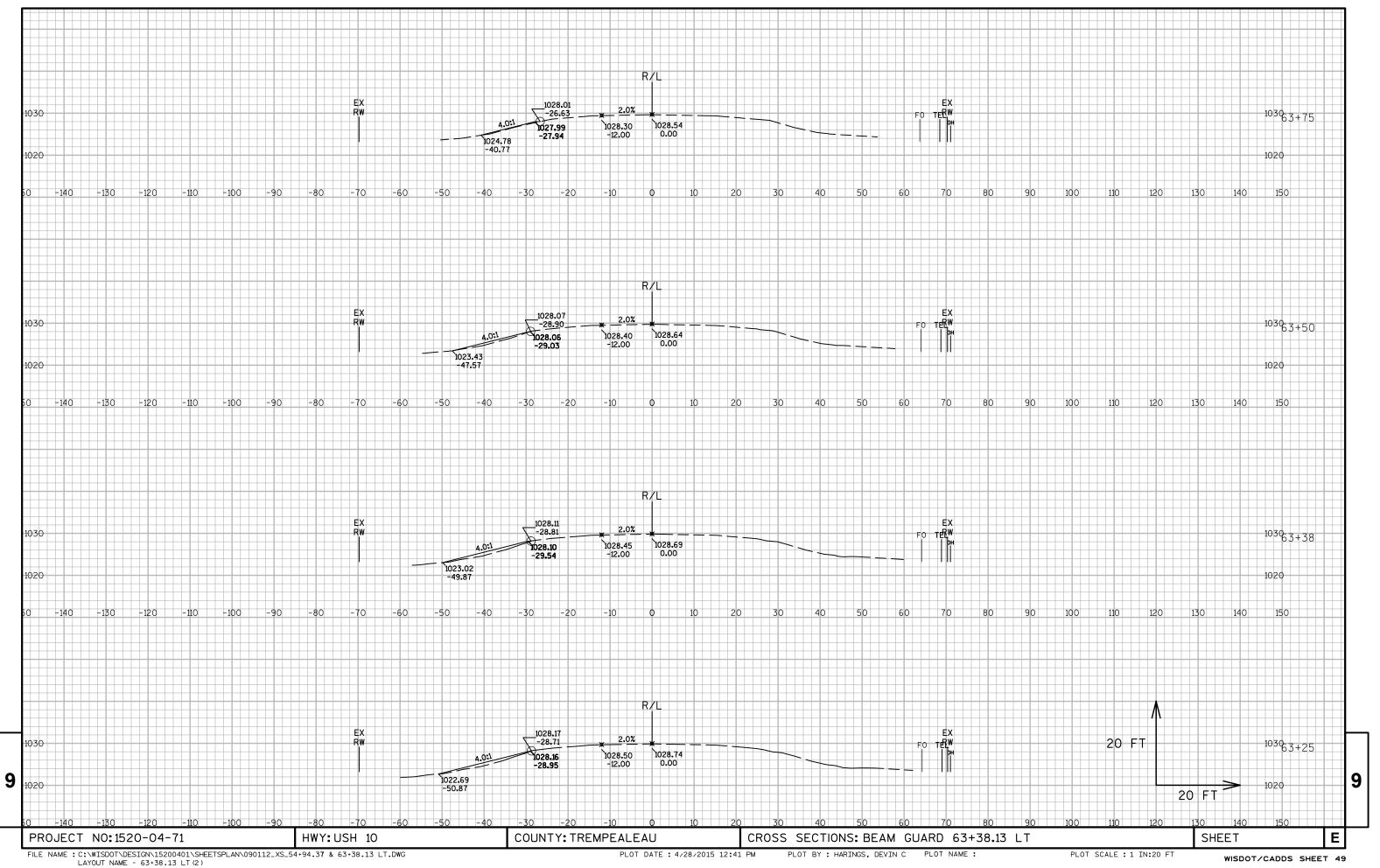


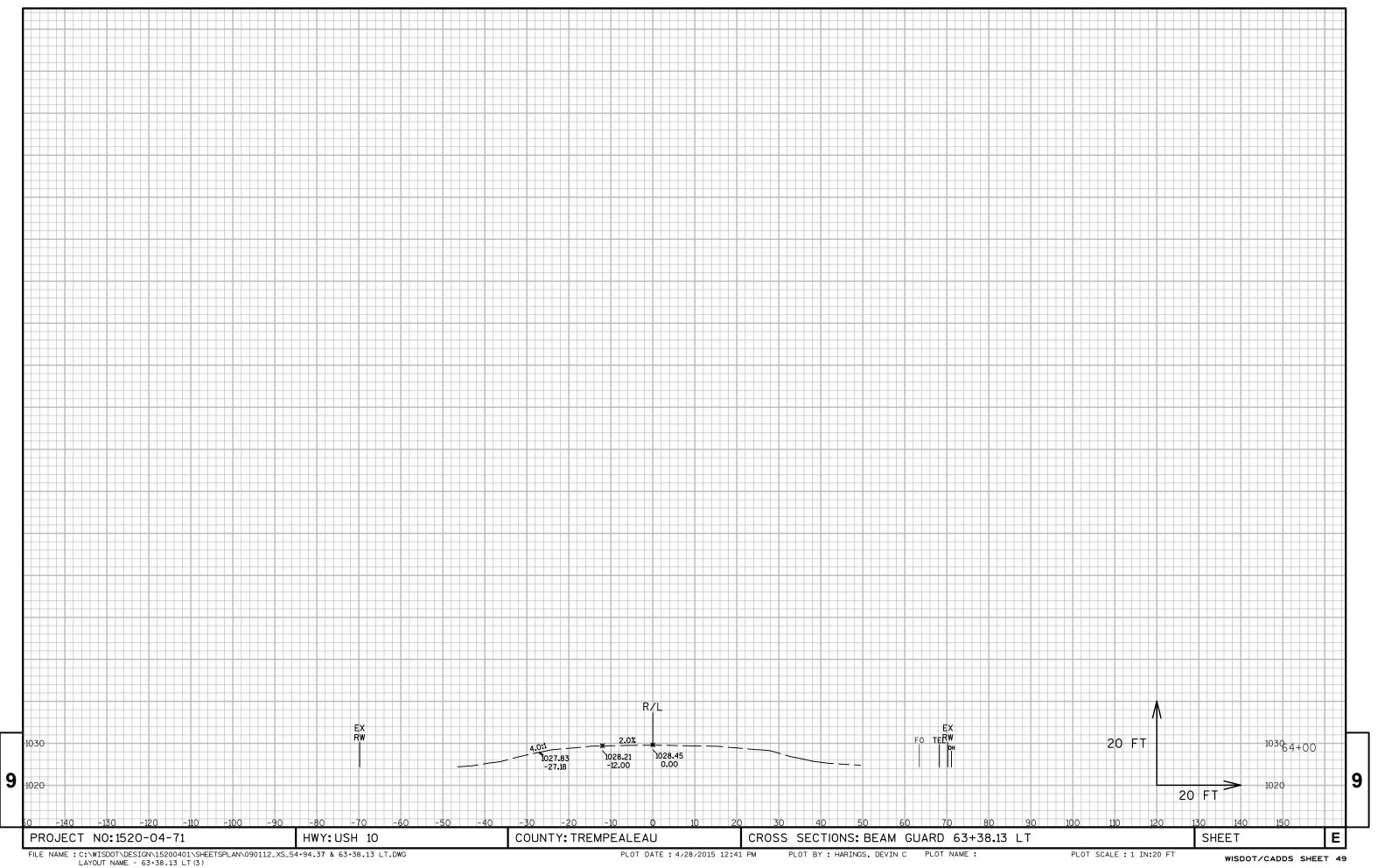


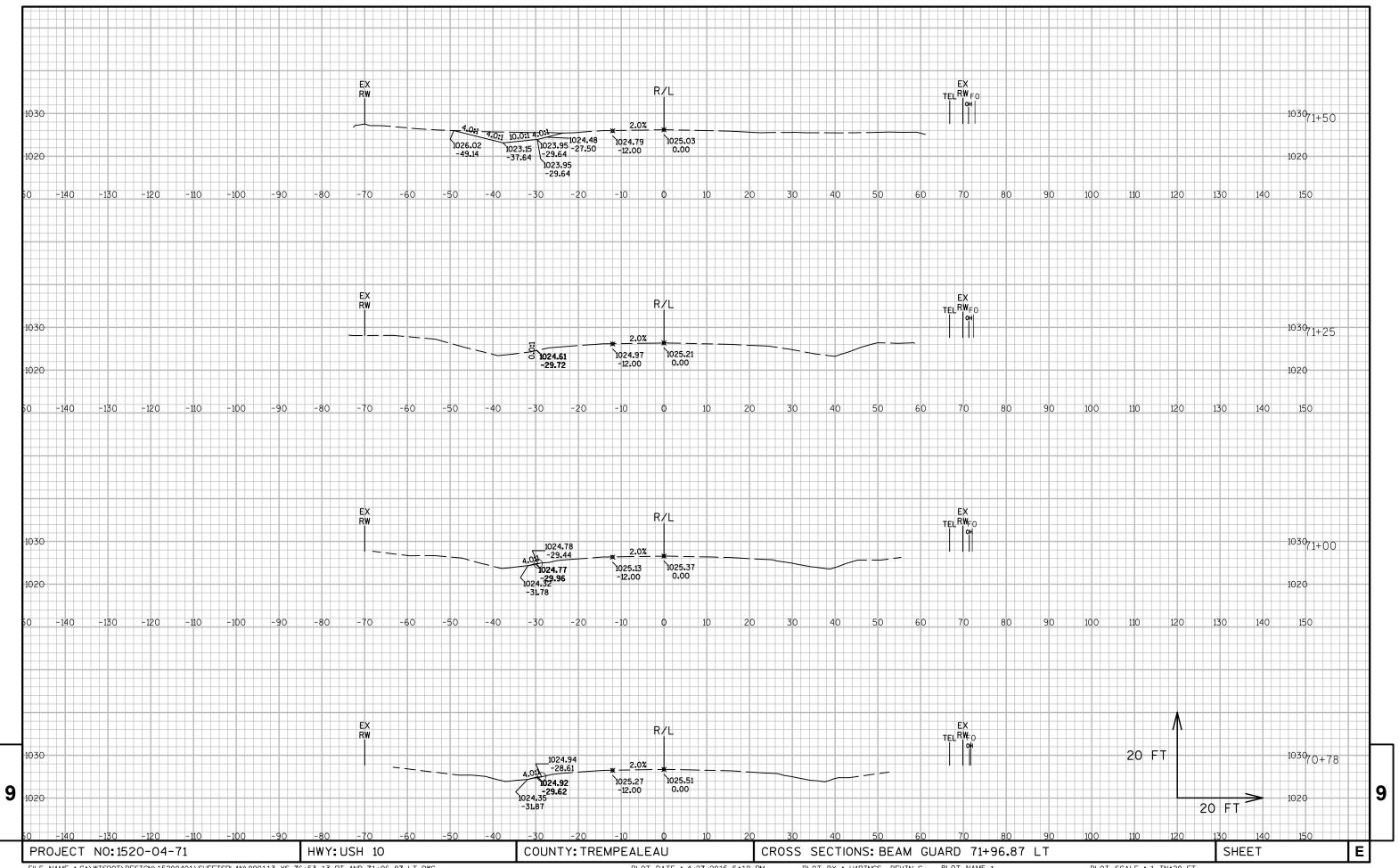


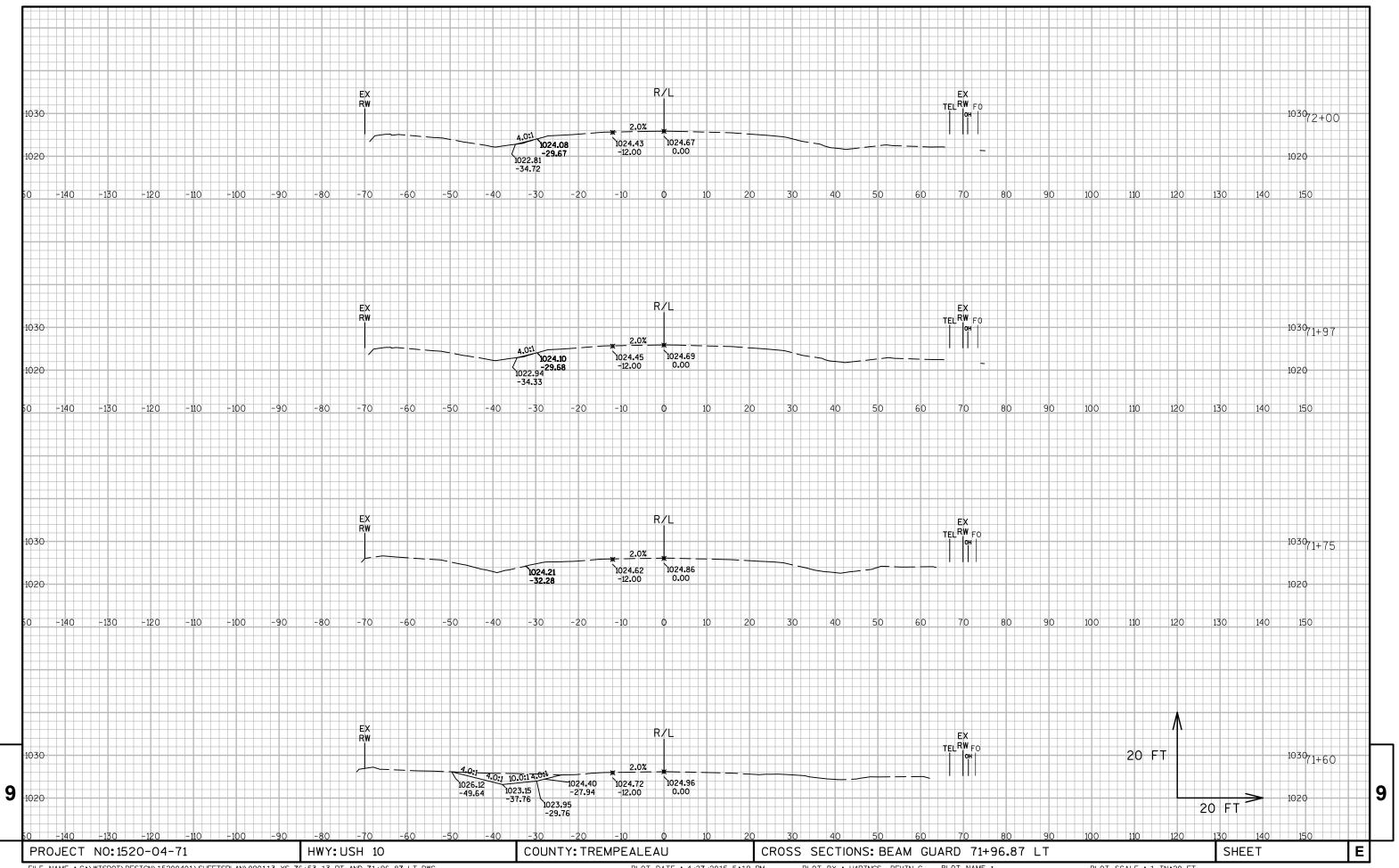


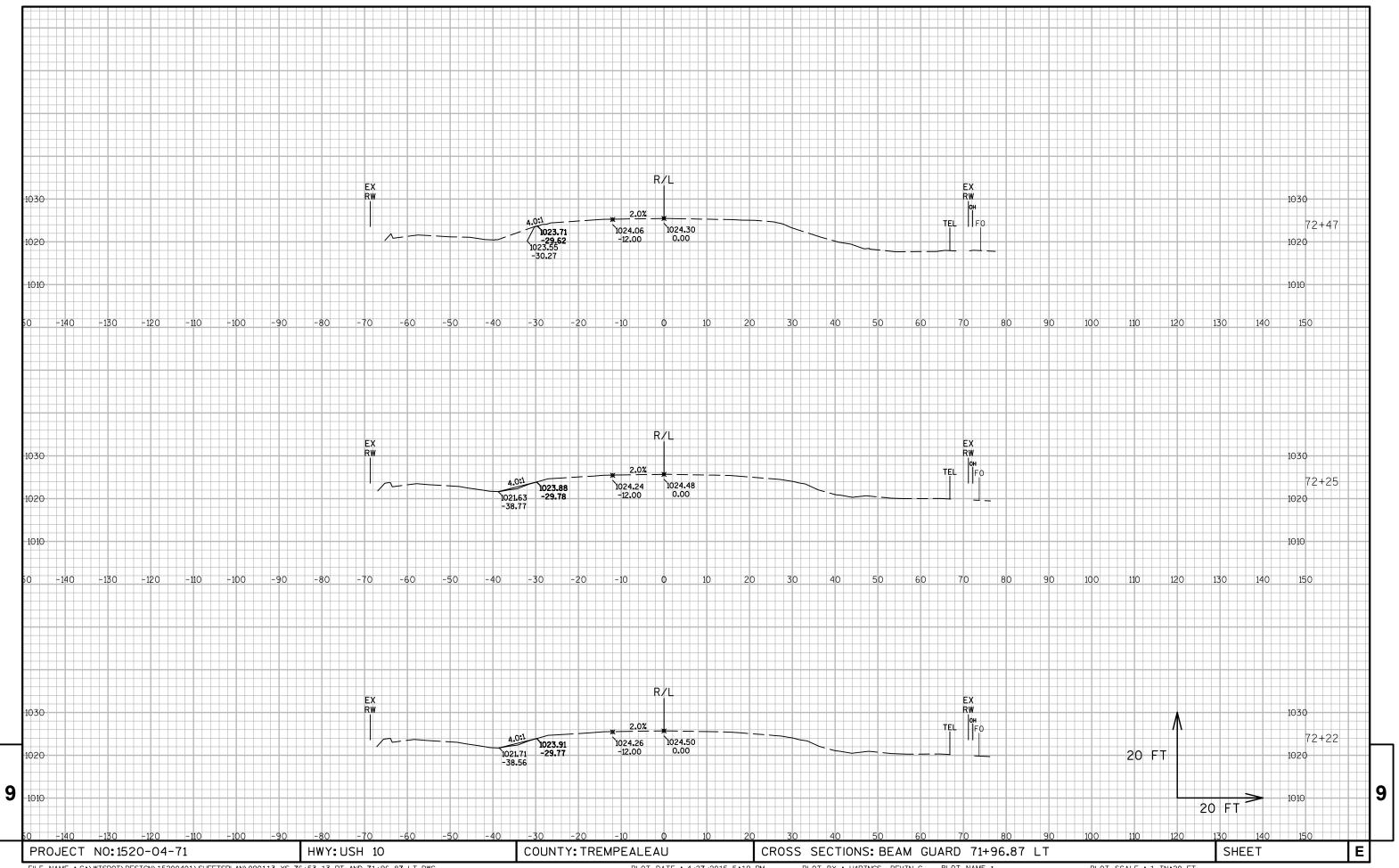


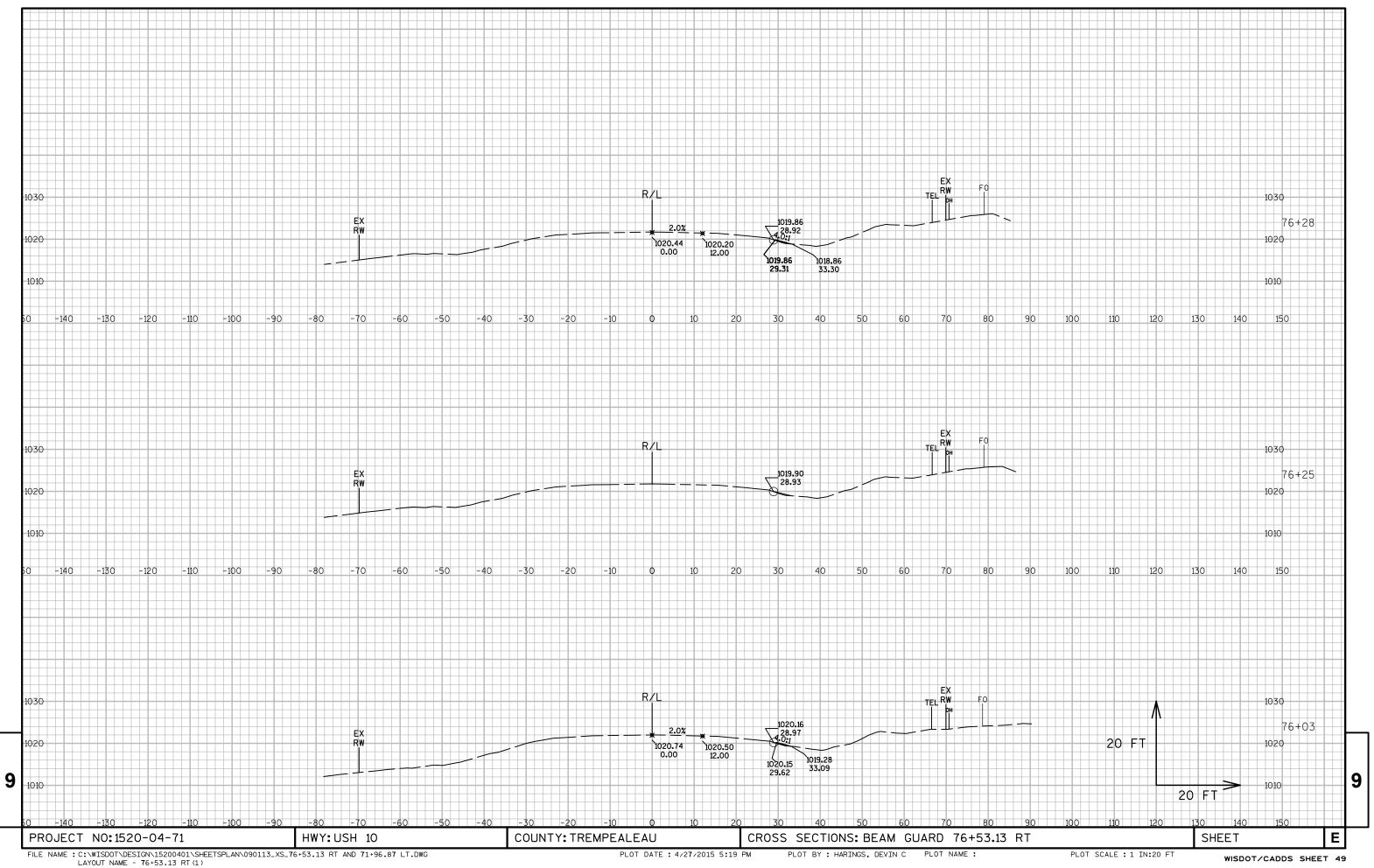


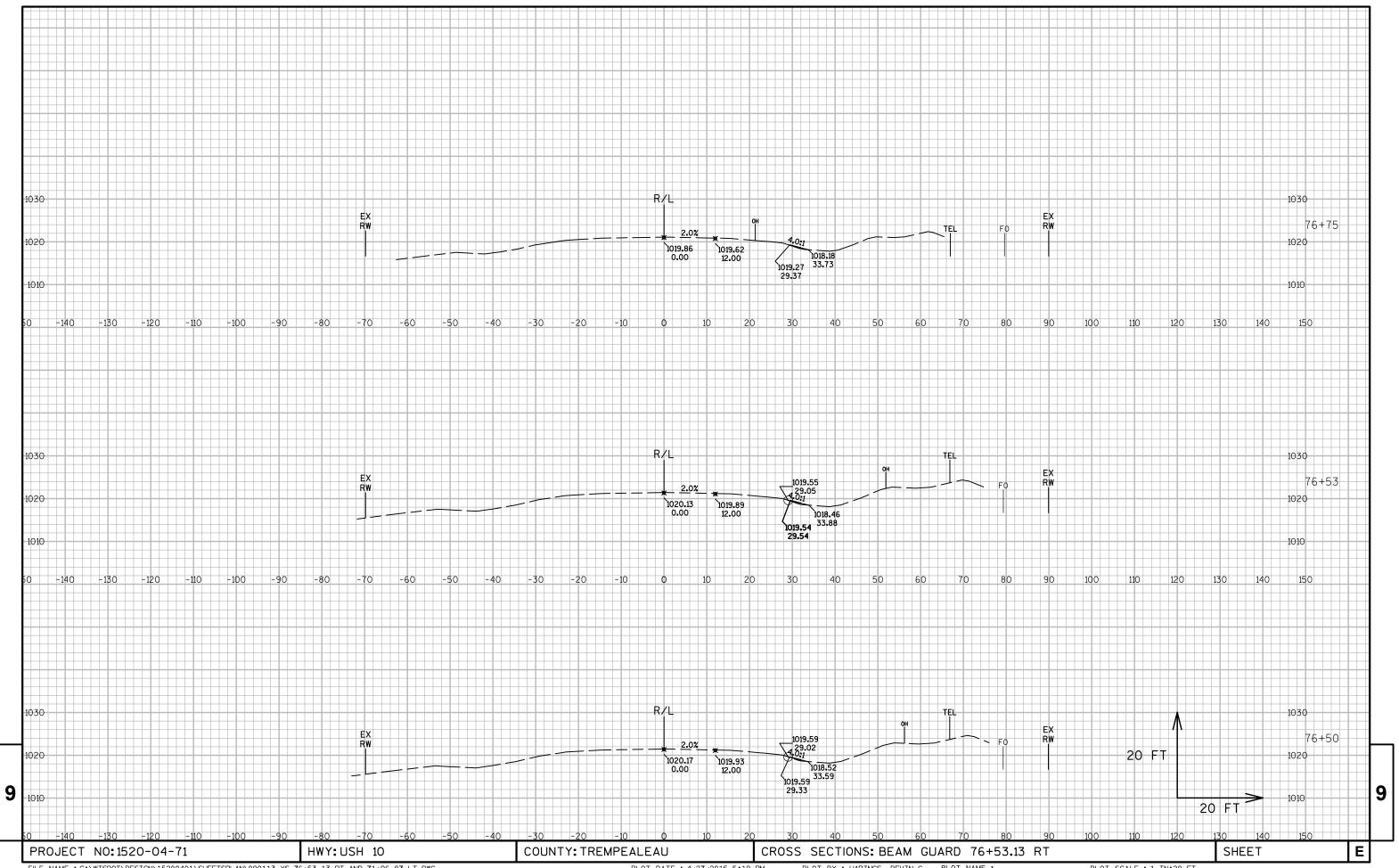


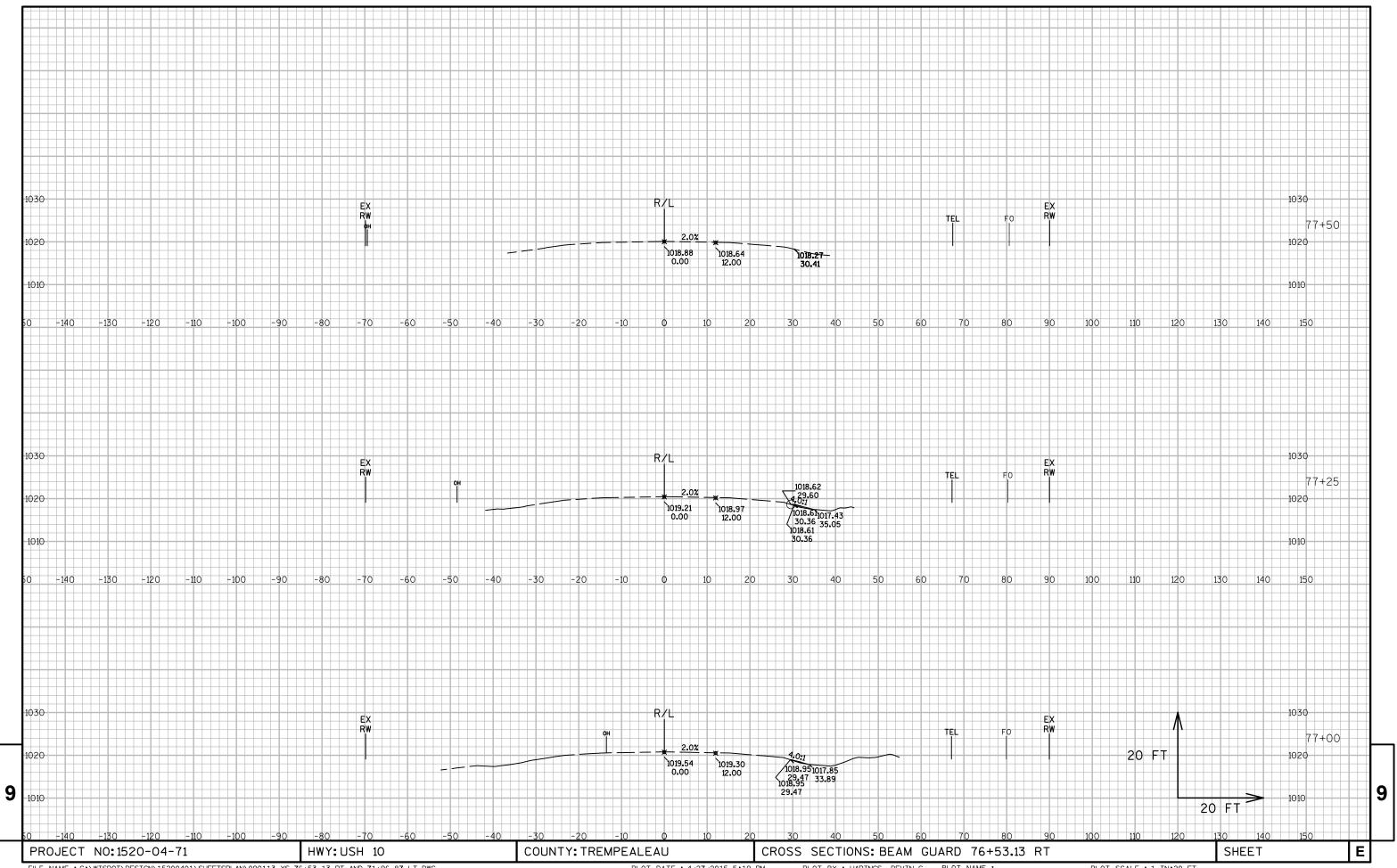


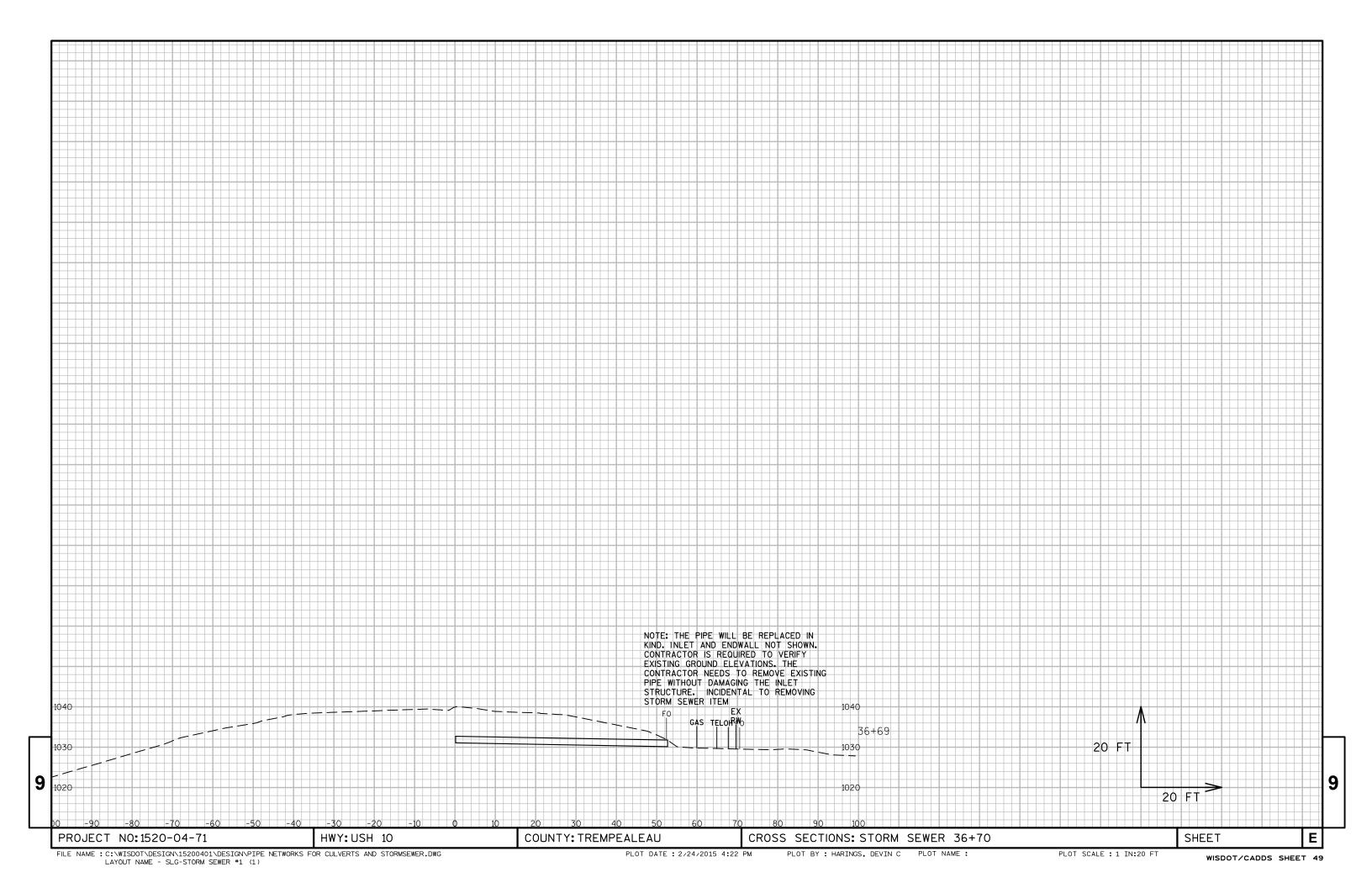


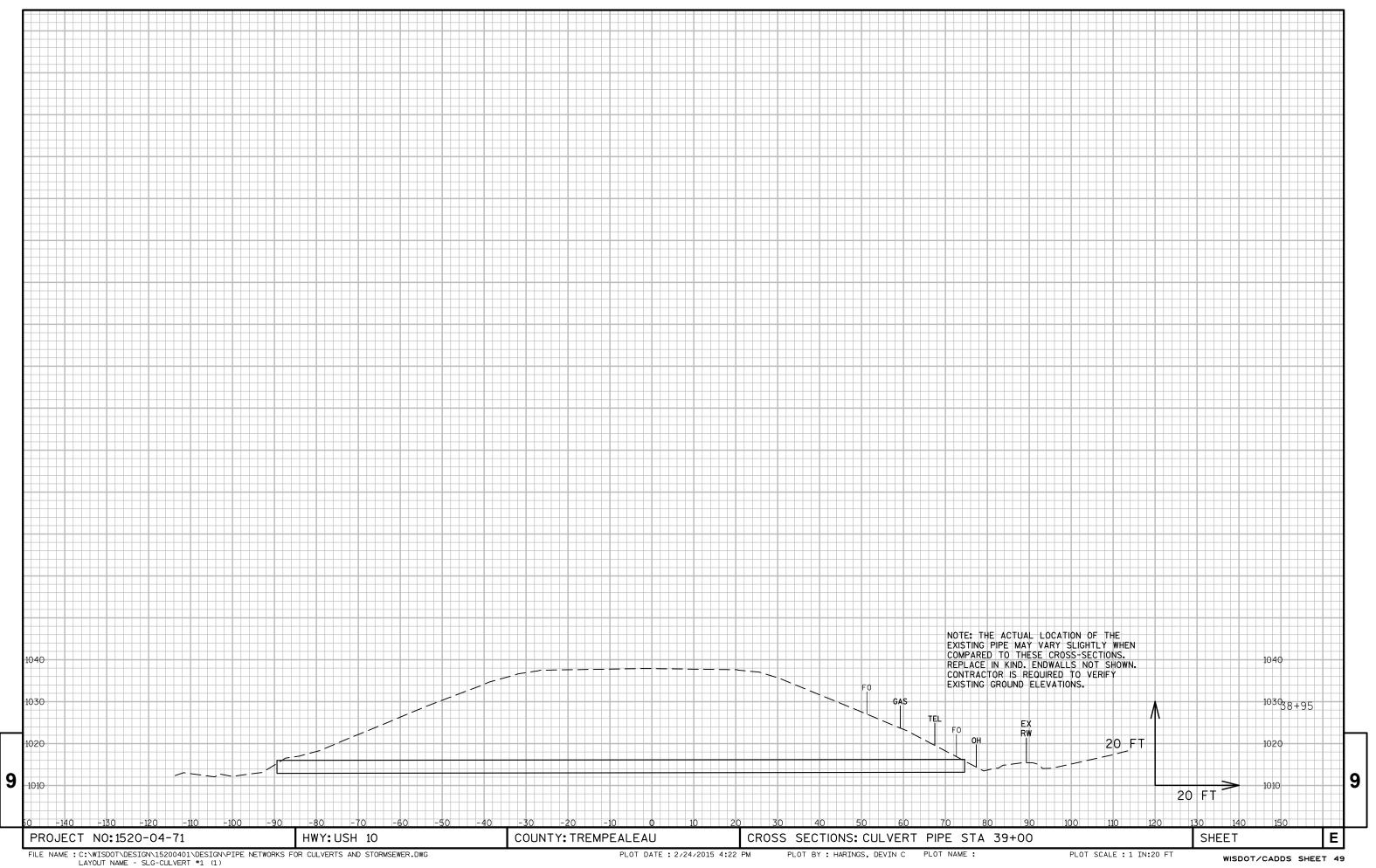


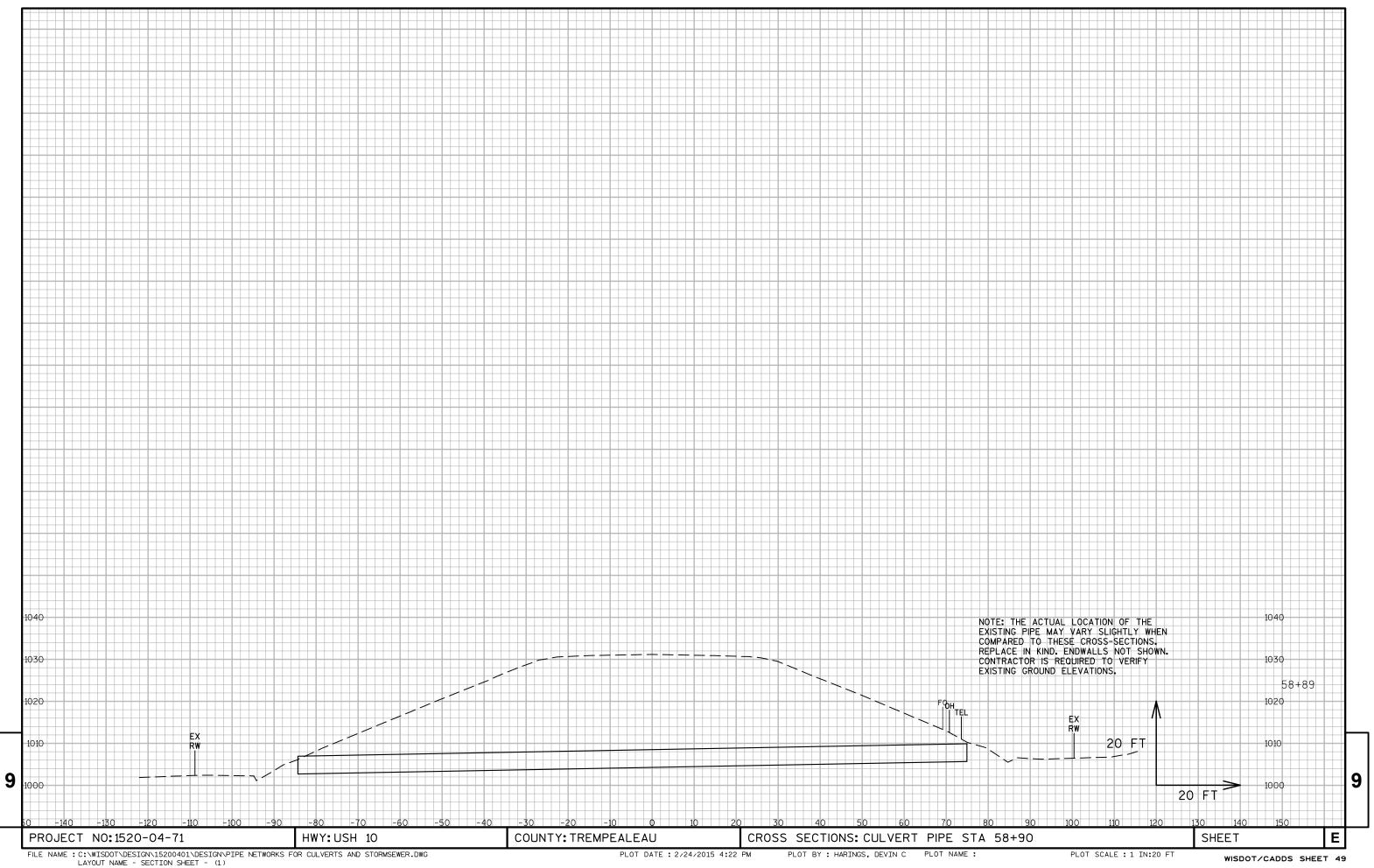


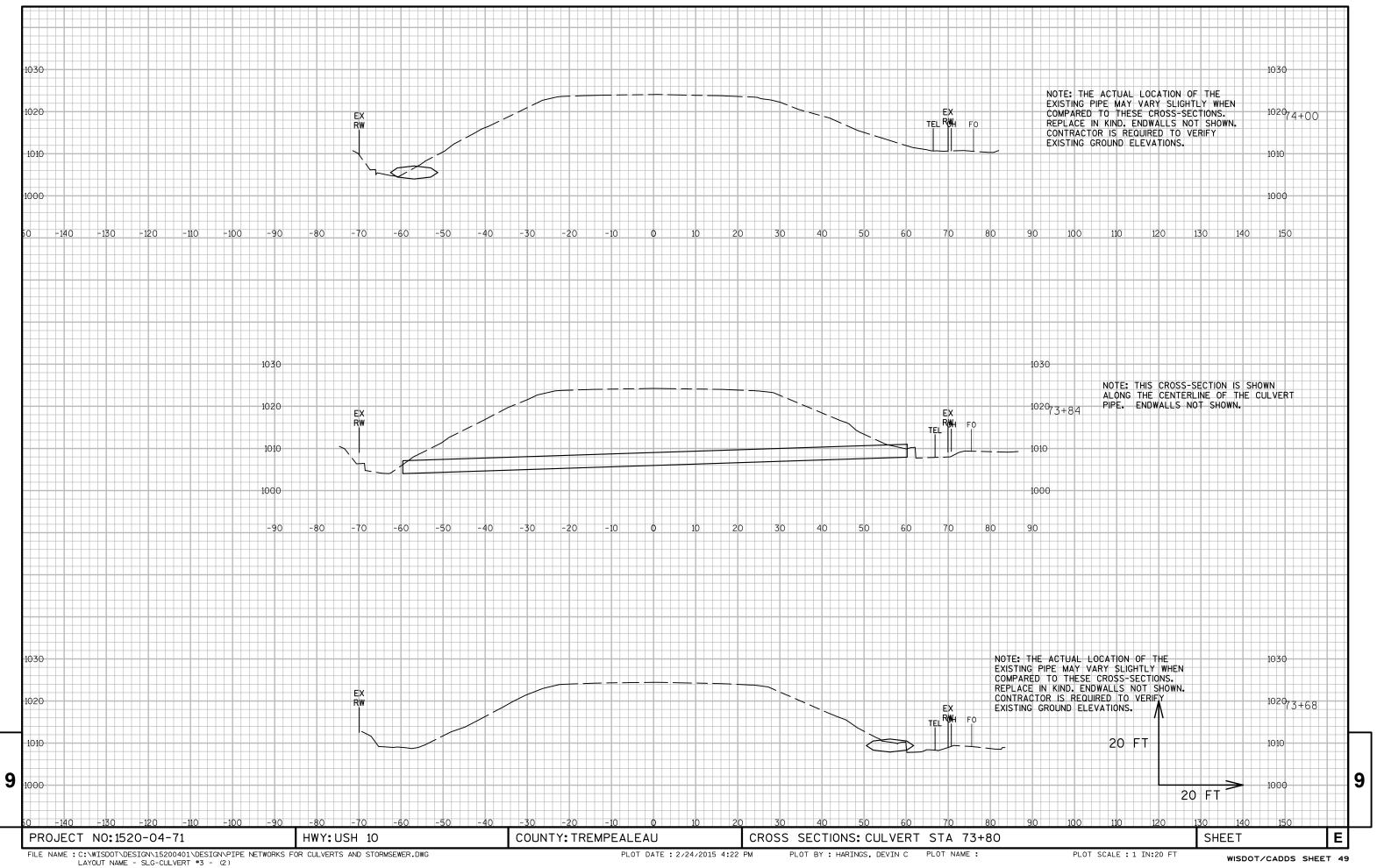












Notes



Wisconsin Department of Transportation

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