

WKE

PROJECT ID: 1300-13-72  
WITH: N/A

COUNTY: MILWAUKEE

NOV 2015  
ORDER OF SHEETS

|               |                              |
|---------------|------------------------------|
| Section No. 1 | Title                        |
| Section No. 2 | Typical Sections and Details |
| Section No. 3 | Estimate of Quantities       |
| Section No. 3 | Miscellaneous Quantities     |
| Section No. 4 | Right of Way Plat            |
| Section No. 5 | Plan and Profile             |
| Section No. 6 | Standard Detail Drawings     |
| Section No. 7 | Sign Plates                  |
| Section No. 8 | Structure Plans              |
| Section No. 9 | Gross Sections               |

TOTAL SHEETS = 44



DESIGN DESIGNATION

|                 |  |
|-----------------|--|
|                 | IH 794<br>CARFERRY DRIVE -<br>LAKE INTERCHANGE |
| A.A.D.T. (2015) | = 22,800 (EB)<br>21,900 (WB)                   |
| A.A.D.T. (2035) | = 26,000 (EB)<br>26,000 (WB)                   |
| D.H.V.          | = 4,100 (EB)<br>4,100 (WB)                     |
| D.D.            | = 78-22  |
| T.              | = 8.4%   |
| DESIGN SPEED    | = 55 MPH                                       |
| ESALS           | = 8,497,200                                    |

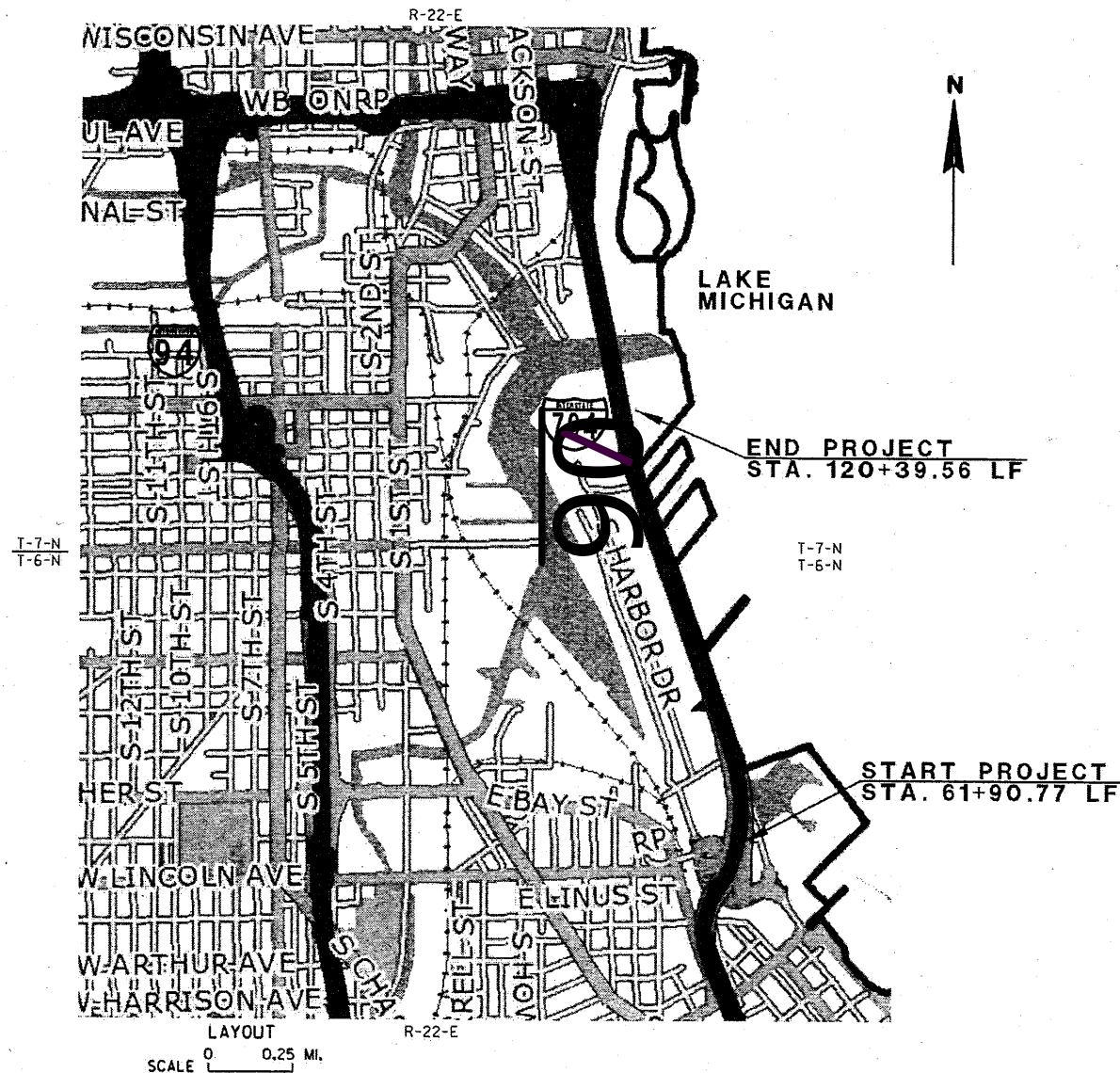
CONVENTIONAL SYMBOLS

|                    |         |
|--------------------|---------|
| PLAN               |         |
| CORPORATE LIMITS   | ////    |
| REFERENCE LINE     | —       |
| COMBUSTIBLE FLUIDS | CAUTION |
| UTILITIES          |         |
| ELECTRIC           | — E —   |
| FIBER OPTIC        | — FO —  |
| GAS                | — G —   |
| SANITARY SEWER     | — SAN — |
| STORM SEWER        | — SS —  |
| TELEPHONE          | — T —   |
| WATER              | — W —   |
| UTILITY PEDESTAL   | ⊗       |
| POWER POLE         | ⊕       |
| TELEPHONE POLE     | ⊙       |

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PLAN OF BRIDGE PAINTING  
HOAN BRIDGE & LAKE FREEWAY  
MILWAUKEE RIVER TO CARFERRY DR  
IH 794  
MILWAUKEE COUNTY

STATE PROJECT NUMBER  
1300-13-72



TOTAL NET LENGTH OF CENTERLINE = 1.107 MI

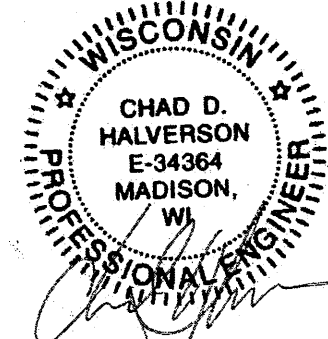
Coordinates on this plan are referenced to the Wisconsin County Coordinate System (WCCS), Milwaukee County, NAD 1983 (2007).  
Elevations shown on this plan are referenced to the North American Vertical Datum of 1988 NAVD 88 (1991).

| STATE PROJECT | FEDERAL PROJECT |          |
|---------------|-----------------|----------|
|               | PROJECT         | CONTRACT |
| 1300-13-72    | WISC 2015468    | 1        |
|               |                 |          |
|               |                 |          |

ORIGINAL PLANS PREPARED BY

Baker

Michael Baker Jr., Inc.  
7633 Ganser Way  
Suite 206  
Madison, WI 53719



5/1/2015  
(Date)

(Signature)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

|                     |                          |
|---------------------|--------------------------|
| PREPARED BY         |                          |
| Surveyor            | KAPUR & ASSOCIATES, INC. |
| Designer            | BAKER CORPORATION        |
| Project Manager     | CAROLYNN GELLINGS, P.E.  |
| Regional Examiner   |                          |
| Regional Supervisor | DEBRA EVERS, P.E.        |
| C.O. Examiner       |                          |

APPROVED FOR THE DEPARTMENT  
DATE: 5/1/2015  
(Signature)

E

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PO BOX 798  
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PHONE(414) 750-2605



Dial 811 or (800) 242-8511  
www.DiggersHotline.com

GENERAL NOTES

CURVE RADII ARE MEASURED TO CURB FLANGE UNLESS OTHERWISE NOTED.

CONTRACTOR SHALL CONTACT SOUTHEASTERN WISCONSIN REGIONAL PLANNING COMMISSION (SEWRPC) BEFORE DISTURBING ANY PUBLIC SURVEY MONUMENTS. CONTACT LEE KREBLIN AT (262) 547-6721 FOR MONUMENT RELOCATION PROCEDURES. CONTRACTOR SHALL ALSO CONTACT TOM LIPSKY AT WISDOT SOUTH EAST REGION AT (262) 548-6737.

THE LOCATION OF KNOWN EXISTING UTILITIES IN THE VICINITY OF THE PROJECT ON THE PLANS IS APPROXIMATE. THERE MAY BE OTHER UTILITIES IN THE AREA THAT ARE NOT SHOWN.

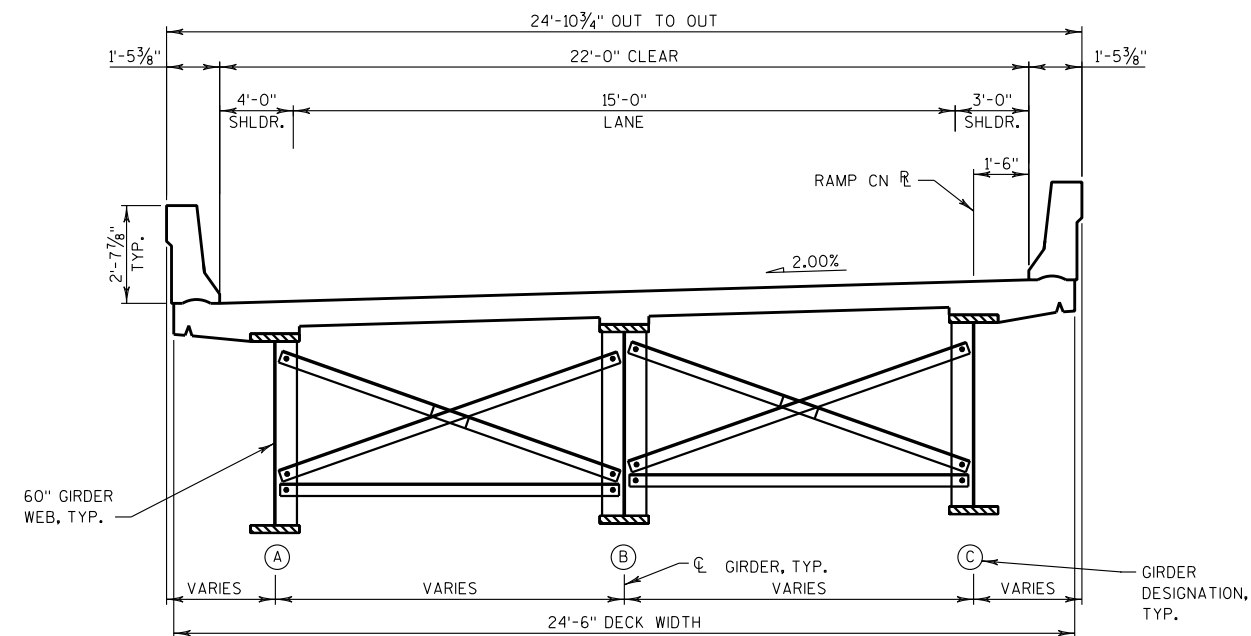
TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

STANDARD ABBREVIATIONS

|       |                                      |
|-------|--------------------------------------|
| BM    | BENCH MARK                           |
| C/L   | CENTER OR CONSTRUCTION LINE          |
| CONC  | CONCRETE                             |
| CATV  | CABLE TELEVISION                     |
| D     | DEGREE OF CURVE                      |
| DES.  | DESIRABLE                            |
| D.S.  | DESIGN SPEED                         |
| DS    | DOWNSPOUT                            |
| EB    | EASTBOUND                            |
| ELECT | ELECTRIC                             |
| ELEV  | ELEVATION                            |
| EPS   | EXTRUDED POLYSTYRENE                 |
| EXIST | EXISTING                             |
| HMA   | HOT MIX ASPHALT                      |
| L     | LENGTH OF CURVE                      |
| LT    | LEFT                                 |
| M/L   | MATCHLINE                            |
| MGS   | MIDWEST GUARDRAIL SYSTEM             |
| MH    | MANHOLE                              |
| MIN.  | MINIMUM                              |
| NB    | NORTHBOUND                           |
| PC    | POINT-OF-CURVE                       |
| PCC   | POINT OF COMPOUND CURVE              |
| PI    | POINT OF INTERSECTION                |
| PM    | PAVEMENT MARKING                     |
| PT    | POINT OF TANGENT                     |
| PVC   | POINT OF VERTICAL CURVE              |
| PVI   | POINT OF VERTICAL INTERSECTION       |
| PVT   | POINT OF VERTICAL TANGENT            |
| R     | RADIUS OF CURVE                      |
| RR    | RAILROAD                             |
| R     | REFERENCE LINE                       |
| R/W   | RIGHT OF WAY                         |
| RC    | REVERSE CROWN                        |
| RT    | RIGHT                                |
| RTRP  | REINFORCED THERMOSETTING RESIN PIPE  |
| S.E.  | SUPER ELEVATION                      |
| S.R.  | SPEED RATING                         |
| SAN   | SANITARY SEWER                       |
| SB    | SOUTHBOUND                           |
| SHLDR | SHOULDER                             |
| SS    | STORM SEWER                          |
| SSPRC | STORM SEWER PIPE REINFORCED CONCRETE |
| STA   | STATION                              |
| T     | TANGENT LENGTH                       |
| TYP   | TYPICAL                              |
| WB    | WESTBOUND                            |

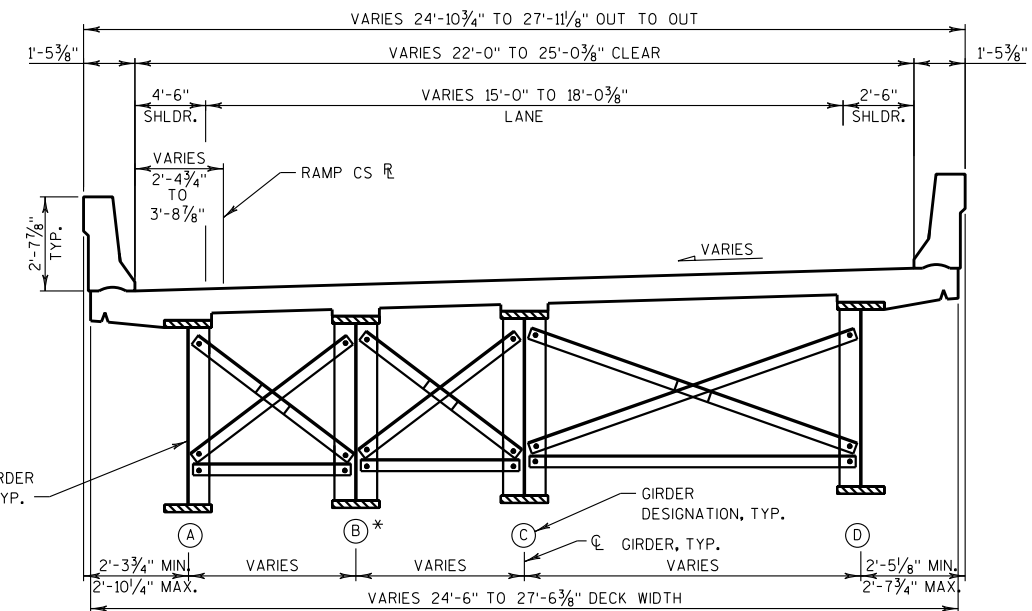
REFERENCE LINE CALLOUTS

| FREEWAY |   | RAMPS |                                   |
|---------|---|-------|-----------------------------------|
| LF      | NORTHBOUND/SOUTHBOUND IH 794 LAKE FREEWAY | CW    | CARFERRY DRIVE WESTBOUND EXIT     |
| NB      | NORTHBOUND IH 794                         | CE    | CARFERRY DRIVE EASTBOUND ENTRANCE |
| SB      | SOUTHBOUND IH 794                         | CS    | CARFERRY DRIVE EASTBOUND EXIT     |
|         |   | CN    | CARFERRY DRIVE WESTBOUND ENTRANCE |



**TYPICAL CROSS SECTION - UNIT S24**

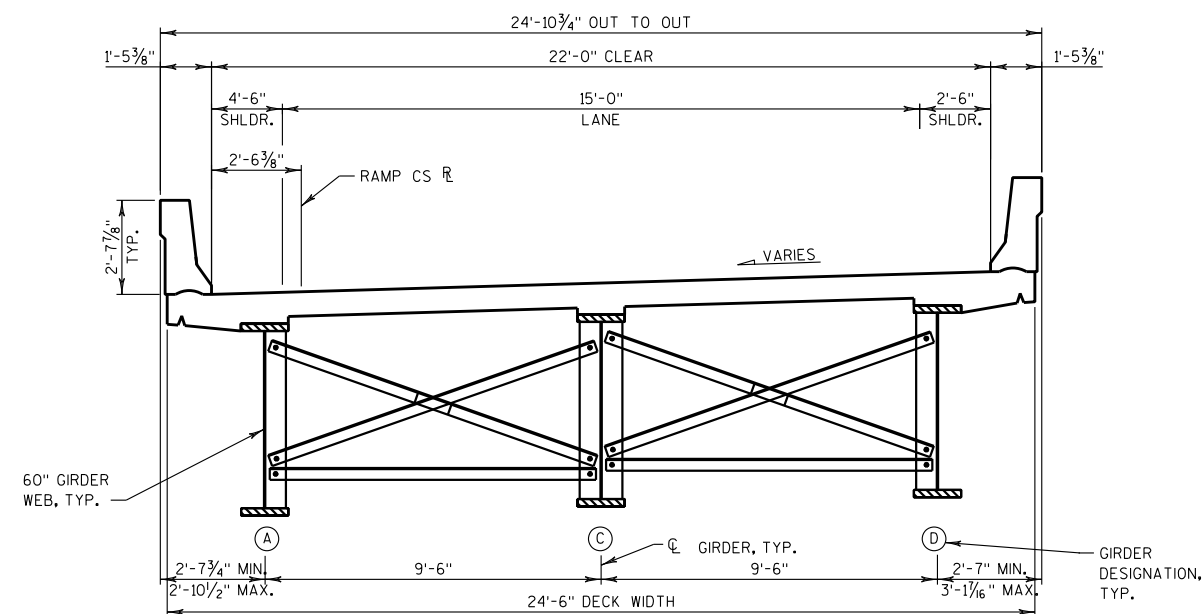
STA. 662+48 CN TO STA. 666+81 CN (LOOKING NORTH)



**TYPICAL CROSS SECTION - UNIT S23**

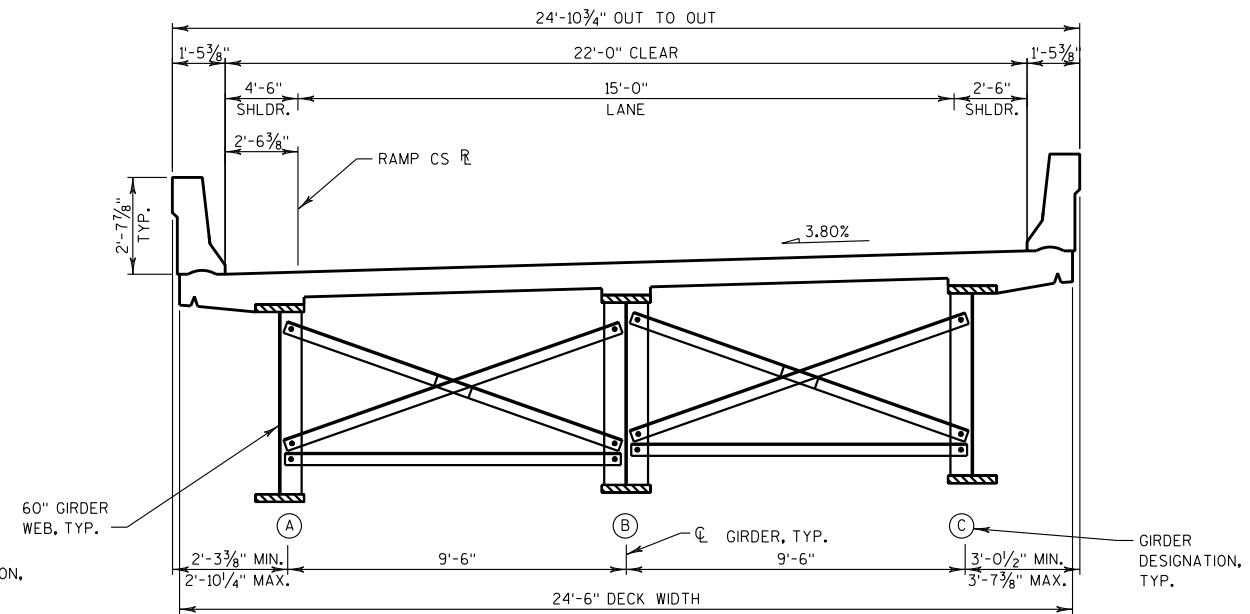
STA. 361+60 CS TO STA. 362+88 CS (LOOKING NORTH)

\* SOUTH END OF SPAN 1 ONLY



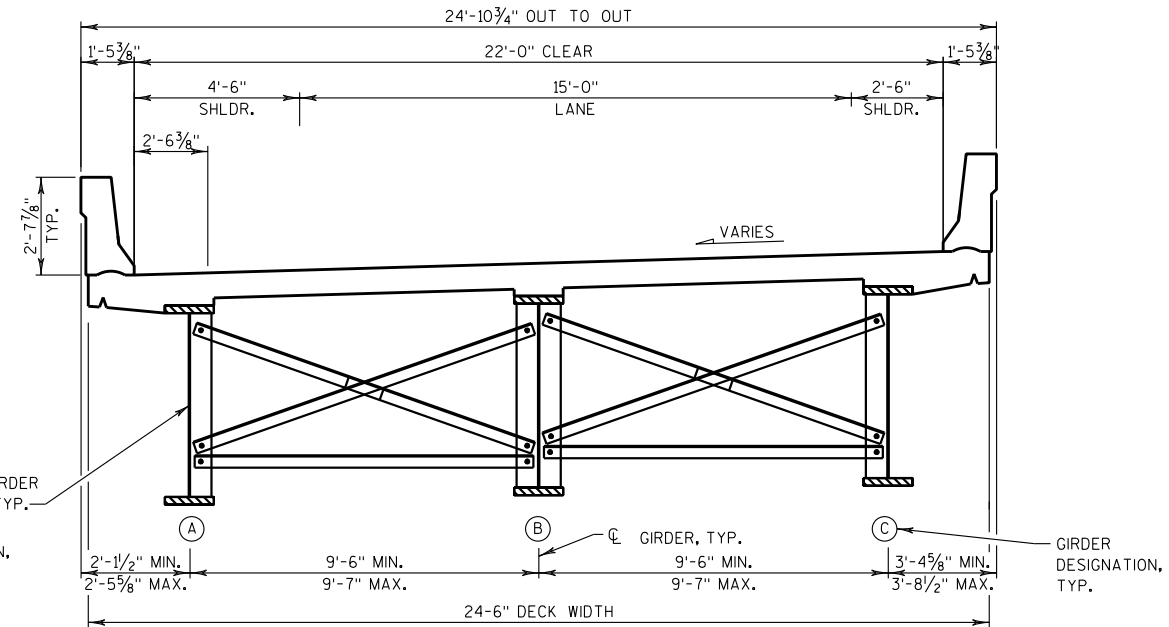
**TYPICAL CROSS SECTION - UNIT S23**

STA. 362+88 CS TO STA. 366+16 CS (LOOKING NORTH)

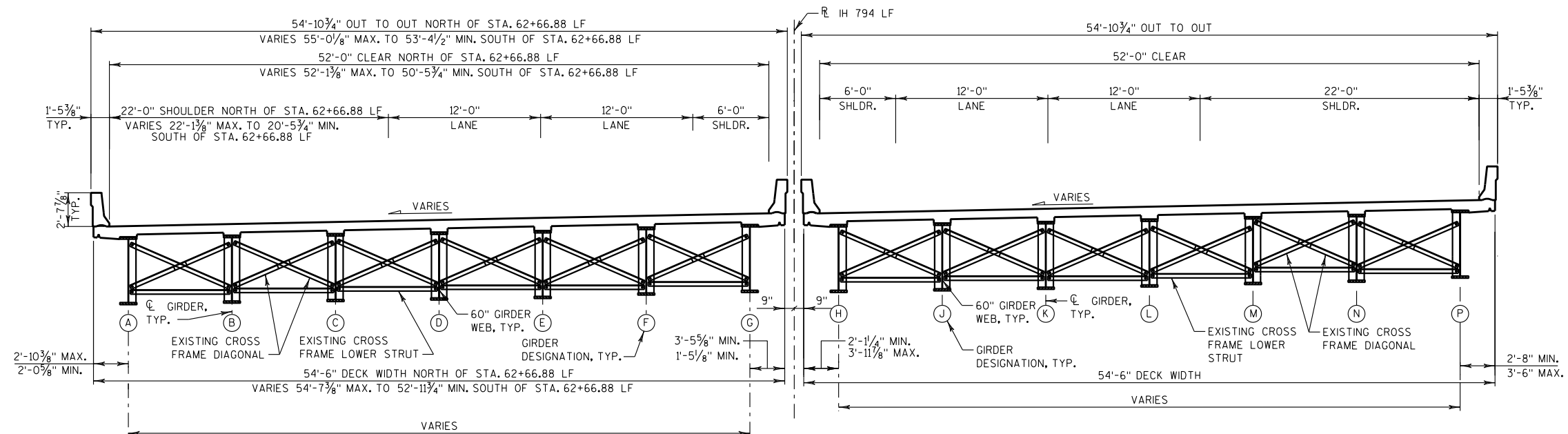


**TYPICAL CROSS SECTION - UNIT S22**

STA. 366+16 CS TO STA. 370+06 CS (LOOKING NORTH)



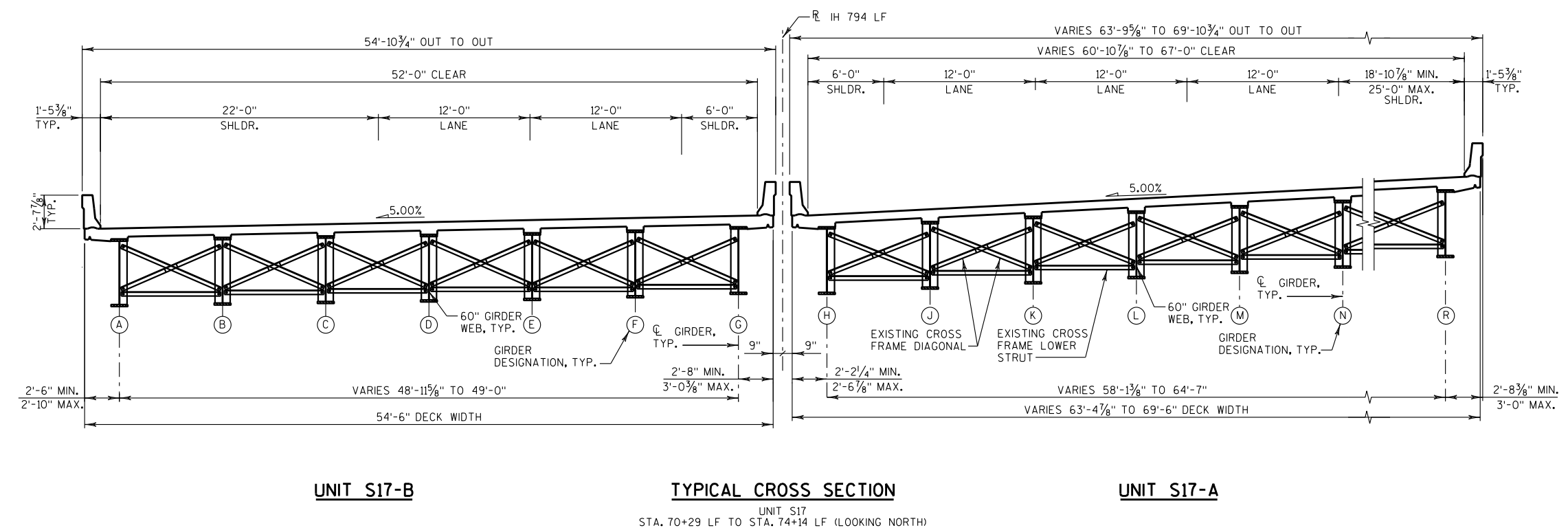
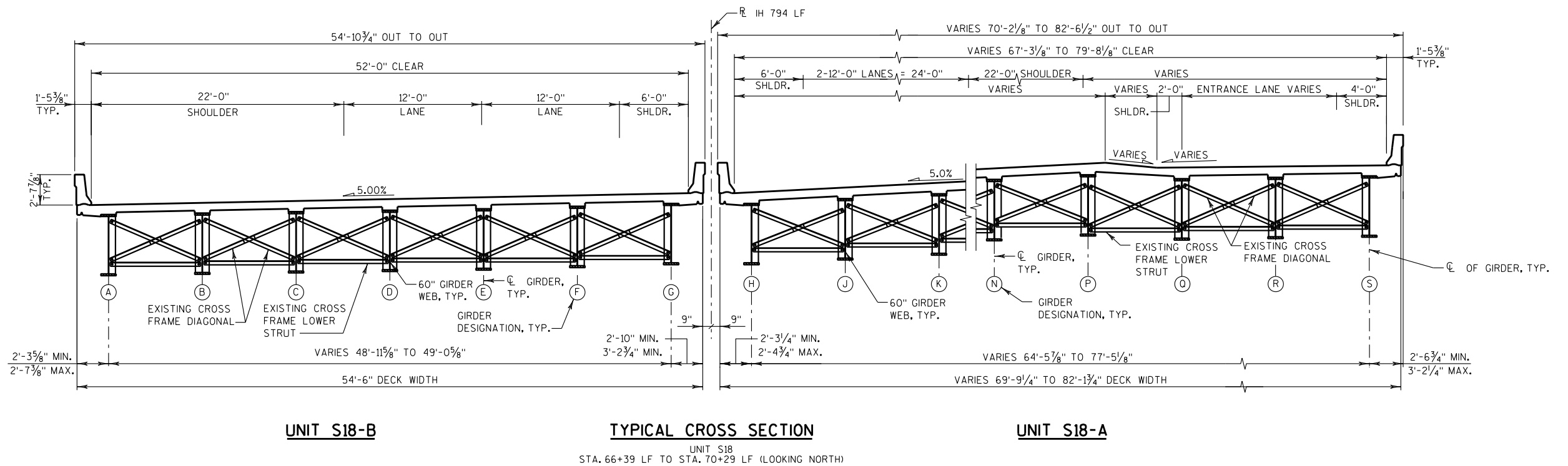
**TYPICAL CROSS SECTION - UNIT S20**  
STA. 373+91 CS TO STA. 377+35 CS (LOOKING NORTH)

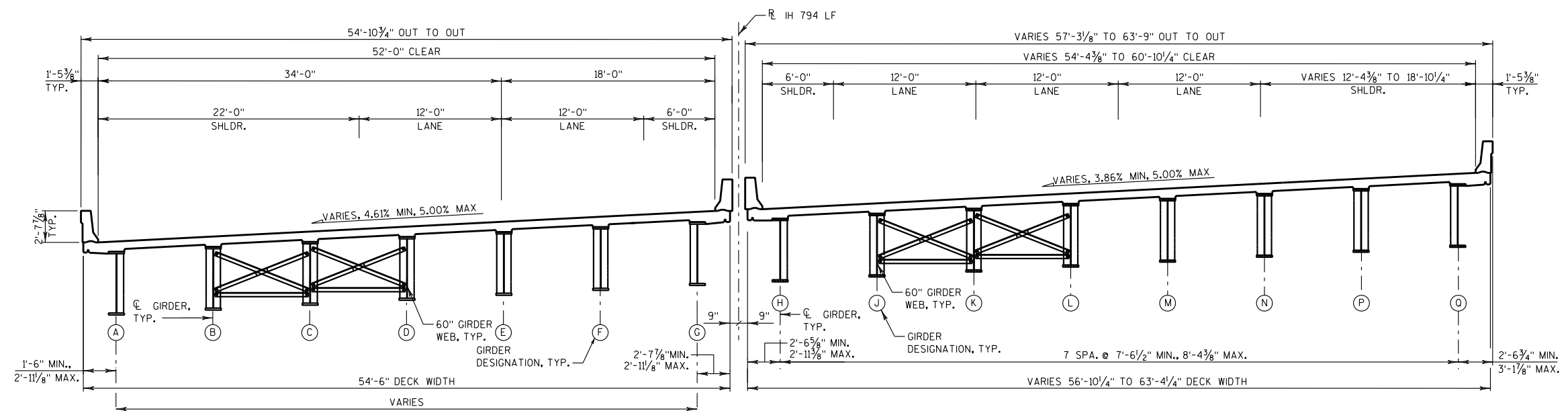


UNIT S19-A

11





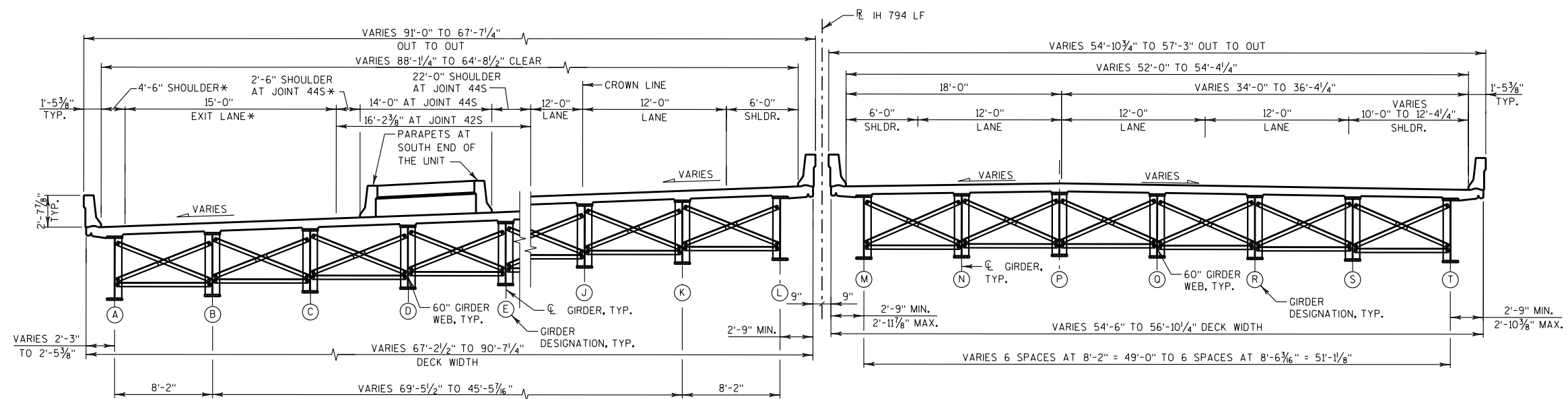


UNIT S16-B

TYPICAL CROSS SECTION

UNIT S16-A

UNIT S16  
STA. 74+14 LF TO STA. 77+59 LF (LOOKING NORTH)

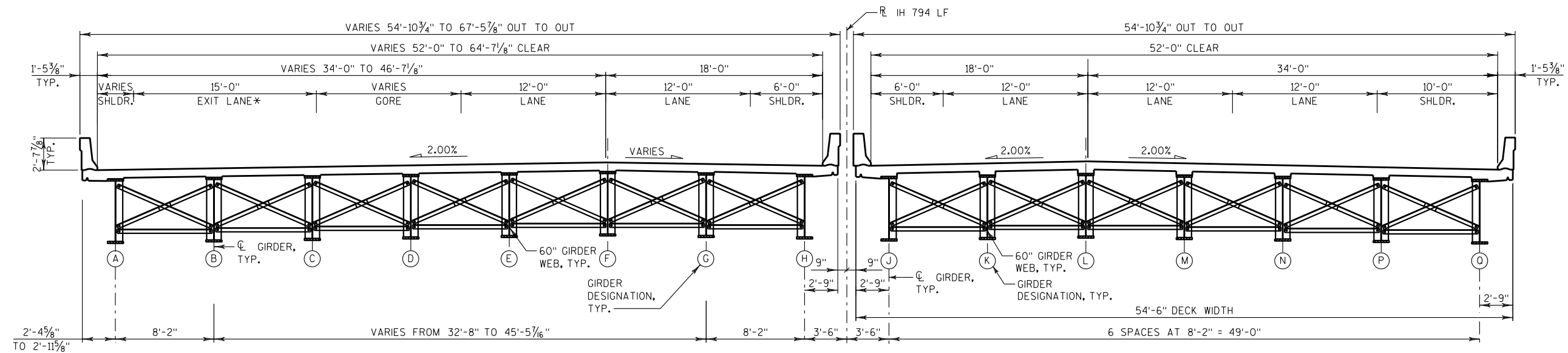


UNIT S15-B

TYPICAL CROSS SECTION

UNIT S15-A

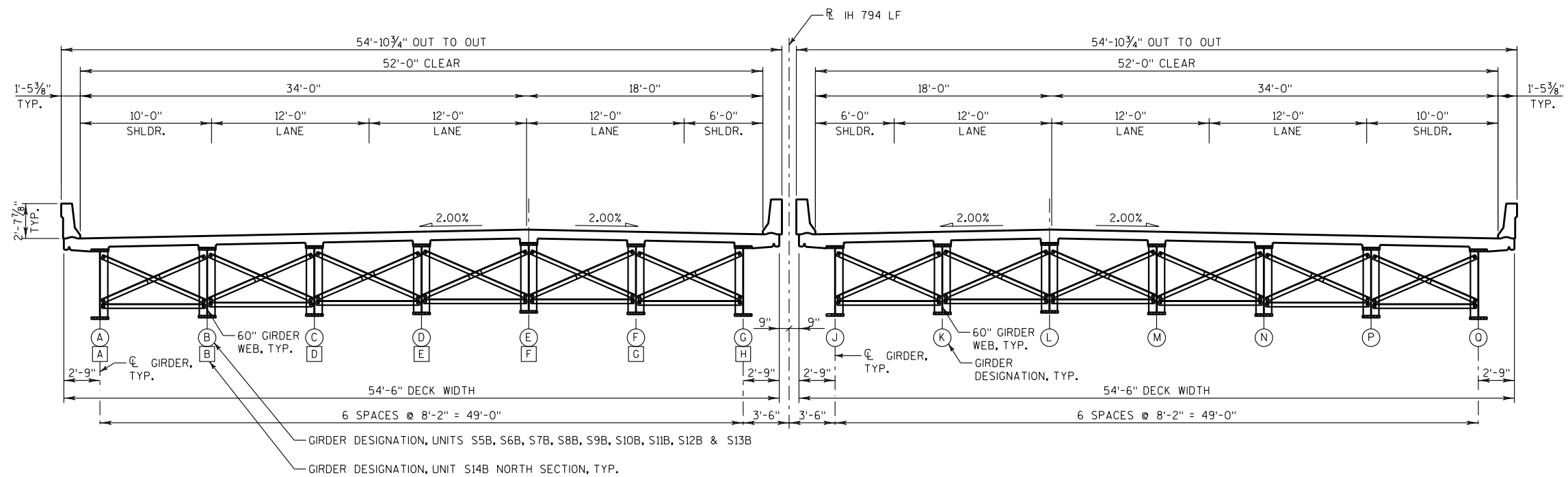
UNIT S15  
STA. 77+59 LF TO STA. 80+59 LF (LOOKING NORTH)  
\*DIMENSION NORMAL TO EXIT LANE



**UNIT S14-B**  
(SOUTH SECTION)

**TYPICAL CROSS SECTION**  
UNIT S14  
STA. 80+59 LF TO STA. 82+16 LF (LOOKING NORTH)

**UNIT S14-A**  
(SOUTH SECTION & NORTH SECTION)

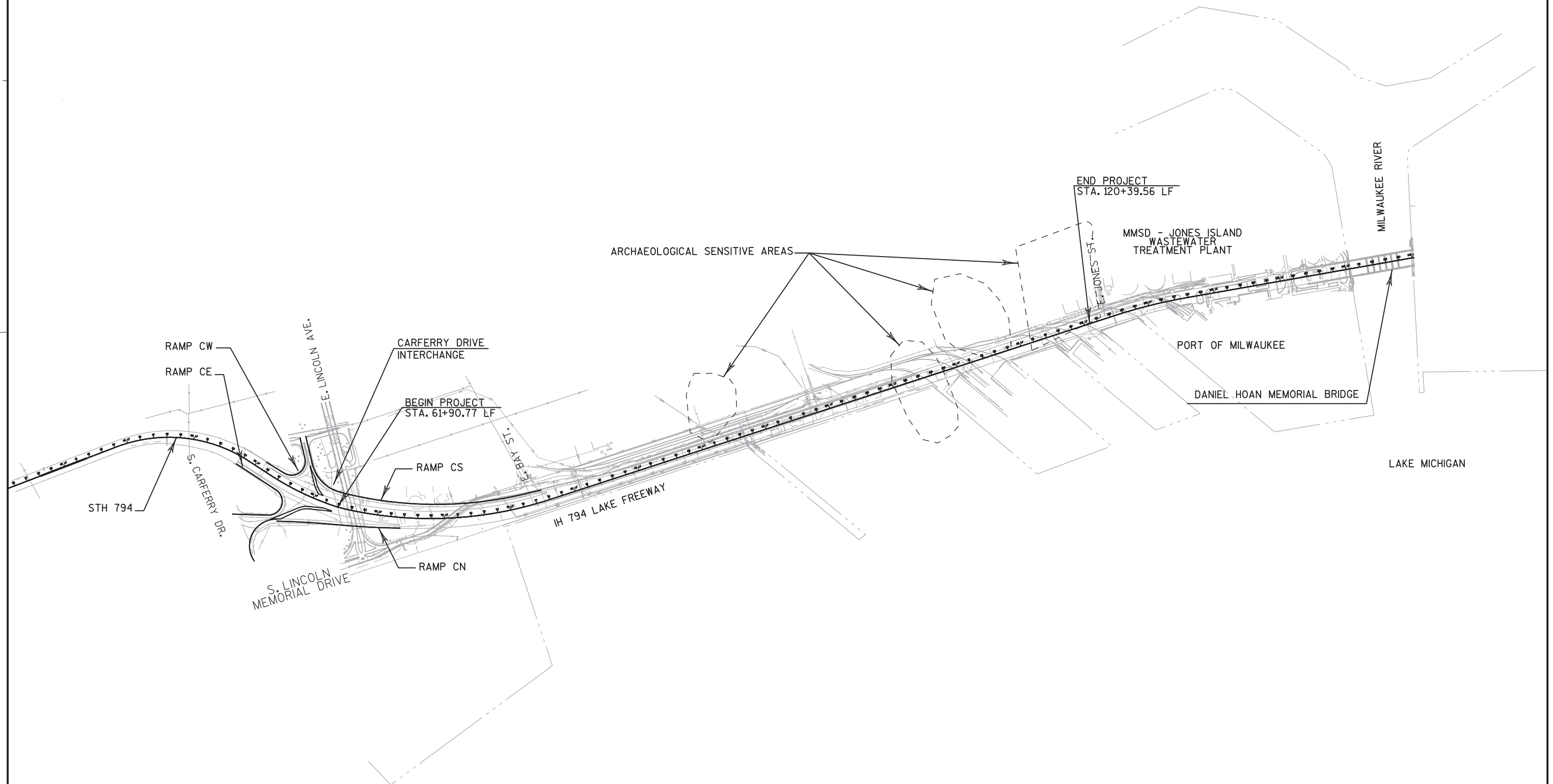


**UNITS S5B, S6B, S7B, S8-B, S9-B, S10-B, S11-B, S12-B, S13-B & S14-B (NORTH SECTION)**

**TYPICAL CROSS SECTION**  
UNITS S5, S6, S7, S8, S9, S10, S11, S12, S13 & S14  
STA. 82+16 LF TO STA. 120+39 LF (LOOKING NORTH)

**UNITS S5A, S6A, S7A, S8-A, S9-A, S10-A, S11-A, S12-A, S13-A, & S14-A**

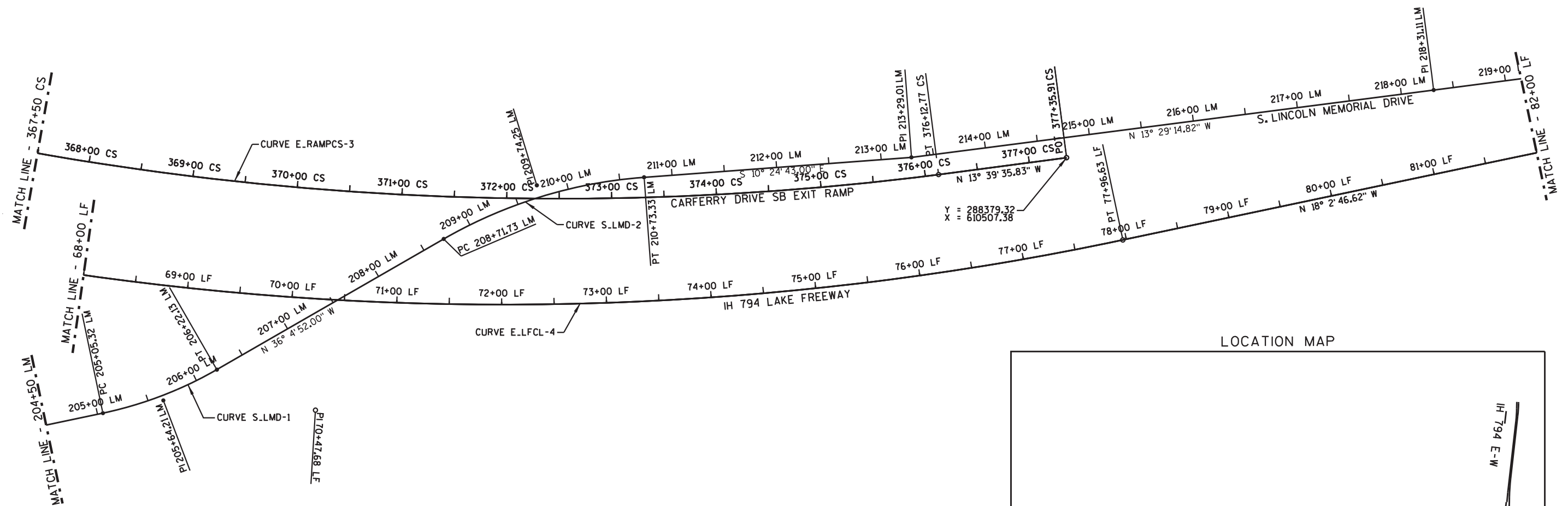




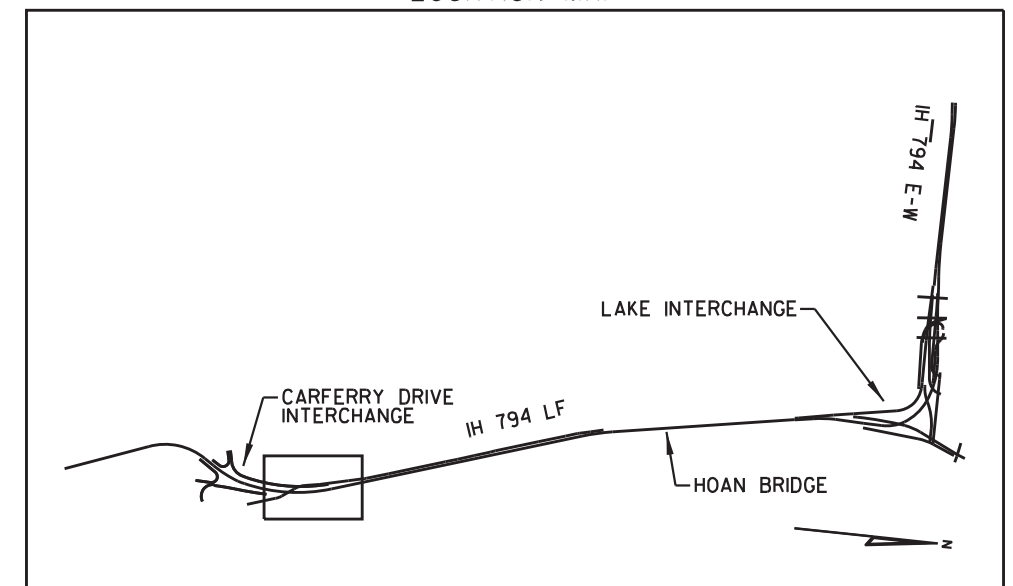


CURVE = E\_RAMPCS-3  
 P.I. = 369+62.38 CS  
 Y = 287,603.55  
 X = 610,692.89  
 DELTA = 26° 29' 13" (LT)  
 D = 1° 59' 57"  
 T = 674.50'  
 L = 1,324.90'  
 R = 2,865.98'  
 P.C. = 362+87.87 CS  
 P.T. = 376+12.77 CS

CURVE = E\_LFCL-4  
 P.I. = 70+50.13 LF  
 Y = 287,695.61  
 X = 610,822.49  
 DELTA = 30° 36' 52" (LT)  
 D = 2° 00' 01"  
 T = 783.99'  
 L = 1,530.50'  
 R = 2,864.38'  
 P.C. = 62+66.13 LF  
 P.T. = 77+96.63 LF



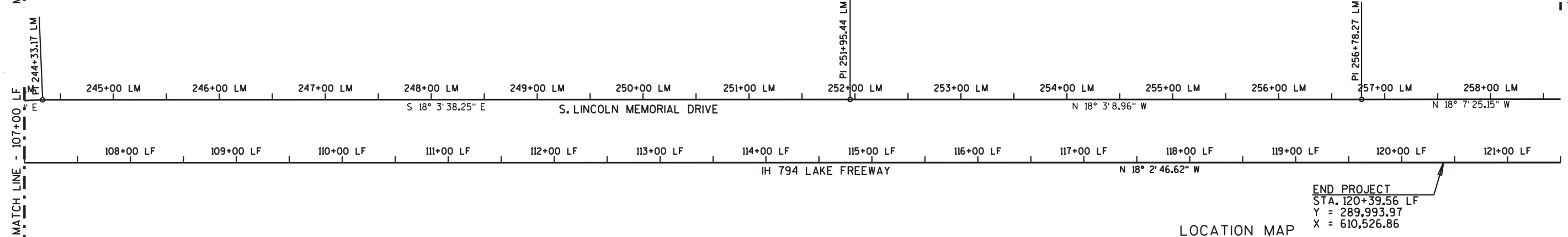
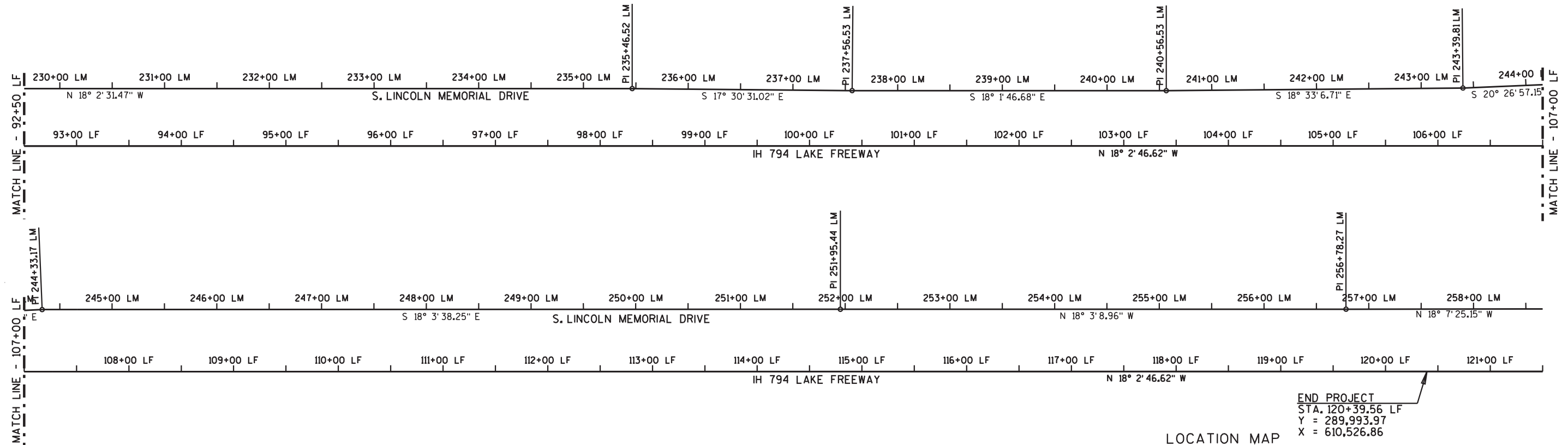
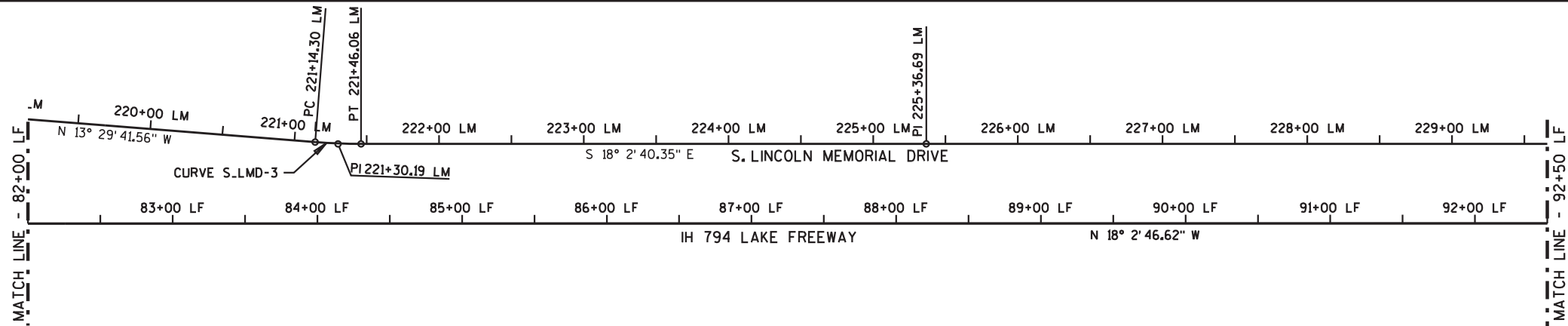
LOCATION MAP



CURVE = S\_LMD-1  
 P.I. = 205+64.21 LM  
 Y = 287,547.70  
 X = 610,829.70  
 DELTA = 18° 05' 19" (LT)  
 D = 15° 29' 07"  
 T = 58.90'  
 L = 116.81'  
 R = 370.00'  
 P.C. = 205+05.32 LM  
 P.T. = 206+22.13 LM

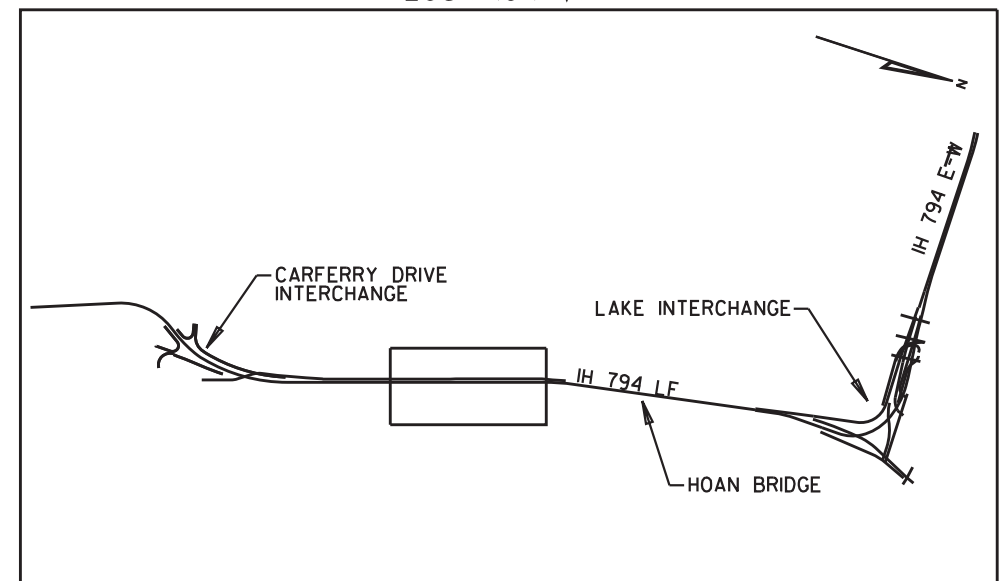
CURVE = S\_LMD-2  
 P.I. = 209+74.25 LM  
 Y = 287,879.88  
 X = 610,587.64  
 DELTA = 25° 40' 09" (RT)  
 D = 12° 43' 57"  
 T = 102.52'  
 L = 201.61'  
 R = 450.00'  
 P.C. = 208+71.73 LM  
 P.T. = 210+73.33 LM

CURVE = S.LMD-3  
P.I. = 221+30.19  
Y = 289,011.26  
X = 610,336.00  
DELTA = 4° 32' 59" (LT)  
D = 14° 19' 26"  
T = 15.89'  
L = 31.76'  
R = 400.00'  
P.C. = 221+14.30  
P.T. = 221+46.06



END PROJECT  
STA. 120+39.56 LF  
Y = 289,993.97  
X = 610,526.86

LOCATION MAP



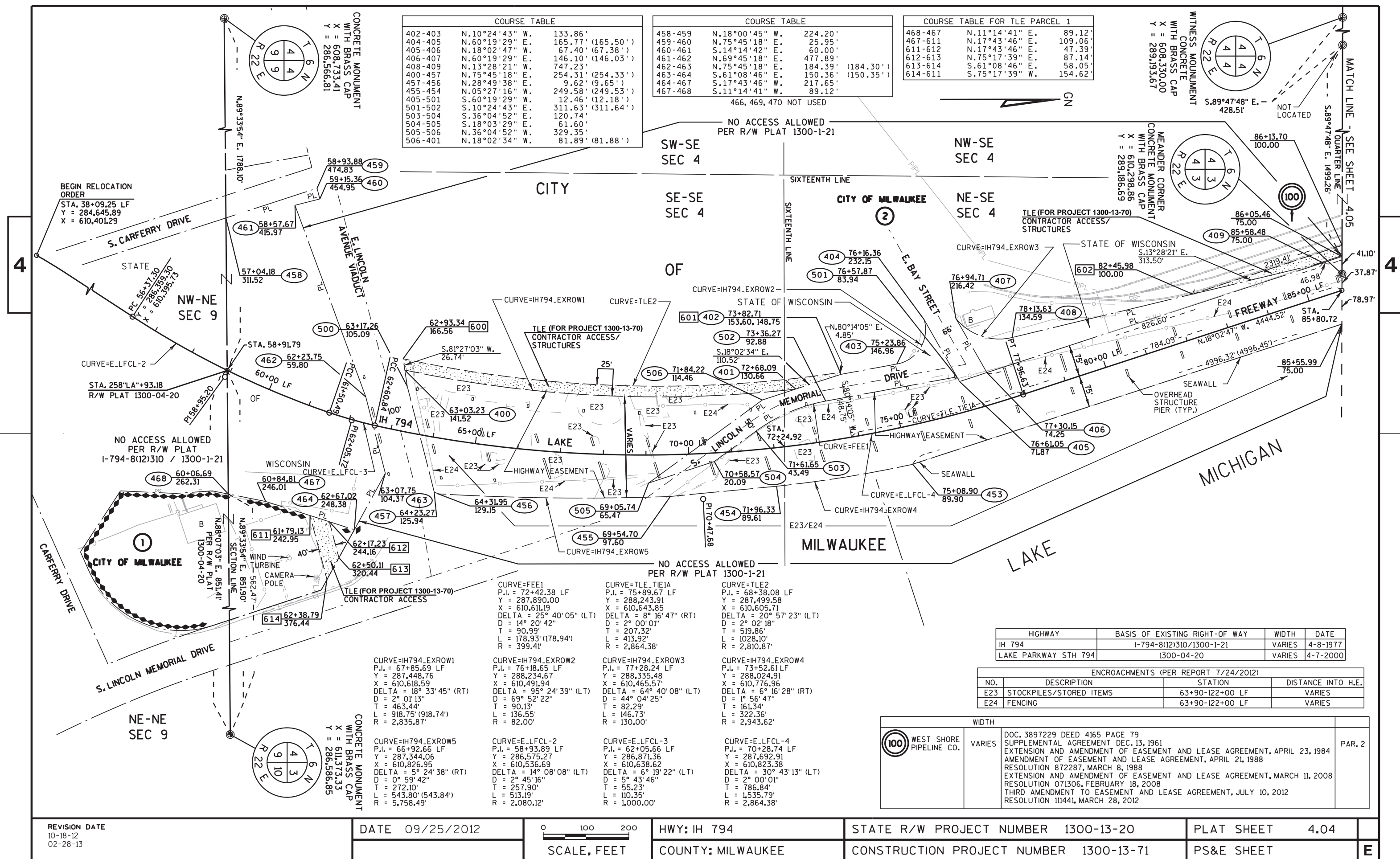
| DATE 14SEP15 |           | E S T I M A T E O F Q U A N T I T I E S                                     |      |             |             |            |
|--------------|-----------|---|------|-------------|-------------|------------|
| LINE         |           |   |      |             |             | 1300-13-72 |
| NUMBER       | ITEM      | ITEM DESCRIPTION  | UNIT | TOTAL       | QUANTITY    |            |
| 0010         | 619. 1000 | Mobilization  | EACH | 1. 000      | 1. 000      |            |
| 0020         | 643. 0100 | Traffic Control (project) 01. 1300-13-72                                    | EACH | 1. 000      | 1. 000      |            |
| 0030         | 643. 0300 | Traffic Control Drums   | DAY  | 1, 999. 000 | 1, 999. 000 |            |
| 0040         | 643. 0420 | Traffic Control Barricades Type III   | DAY  | 53. 000     | 53. 000     |            |
| 0050         | 643. 0705 | Traffic Control Warning Lights Type A                                       | DAY  | 53. 000     | 53. 000     |            |
|              |           |   |      |             |             |            |
| 0060         | 643. 0715 | Traffic Control Warning Lights Type C                                       | DAY  | 595. 000    | 595. 000    |            |
| 0070         | 643. 0800 | Traffic Control Arrow Boards  | DAY  | 81. 000     | 81. 000     |            |
| 0080         | 643. 0900 | Traffic Control Signs   | DAY  | 318. 000    | 318. 000    |            |
| 0090         | 643. 1050 | Traffic Control Signs PCMS  | DAY  | 360. 000    | 360. 000    |            |
| 0100         | 643. 3000 | Traffic Control Detour Signs  | DAY  | 1, 005. 000 | 1, 005. 000 |            |
|              |           |   |      |             |             |            |
| 0110         | ASP. 1T0A | On-the-Job Training Apprentice at \$5. 00/HR                                | HRS  | 2, 000. 000 | 2, 000. 000 |            |
| 0120         | ASP. 1T0G | On-the-Job Training Graduate at \$5. 00/HR                                  | HRS  | 6, 000. 000 | 6, 000. 000 |            |
| 0130         | SPV. 0060 | Special 01. Replace Bolts   | EACH | 60. 000     | 60. 000     |            |
| 0140         | SPV. 0075 | Special 01. Safety Spotter  | HRS  | 3, 000. 000 | 3, 000. 000 |            |
| 0150         | SPV. 0105 | Special 01. Structure Repainting<br>Recycled Abrasive Project 1300-13-72    | LS   | 1. 000      | 1. 000      |            |
|              |           |   |      |             |             |            |
| 0160         | SPV. 0105 | Special 02. Painting Warranted Project 1300-13-72                           | LS   | 1. 000      | 1. 000      |            |
| 0170         | SPV. 0105 | Special 03. Negative Pressure Containment And Collection Of Waste Materials | LS   | 1. 000      | 1. 000      |            |
| 0180         | SPV. 0105 | Special 04. Portable Decontamination Facilities Project 1300-13-72          | LS   | 1. 000      | 1. 000      |            |

| TRAFFIC CONTROL |               |    |         |      |          |       |                 |      |                 |      |                 |      |              |      |          |      |          |       |
|-----------------|---------------|----|---------|------|----------|-------|-----------------|------|-----------------|------|-----------------|------|--------------|------|----------|------|----------|-------|
|                 |               |    |         |      | 643.0300 |       | 643.0420        |      | 643.0705        |      | 643.0715        |      | 643.0800     |      | 643.0900 |      | 643.3000 |       |
|                 |               |    |         |      | TRAFFIC  |       | TRAFFIC CONTROL |      | TRAFFIC CONTROL |      | TRAFFIC CONTROL |      | TRAFFIC      |      | TRAFFIC  |      | TRAFFIC  |       |
|                 |               |    |         |      | CONTROL  |       | BARRICADES      |      | WARNING LIGHTS  |      | WARNING LIGHTS  |      | CONTROL      |      | CONTROL  |      | CONTROL  |       |
|                 |               |    |         |      | DRUMS    |       | TYPE III        |      | TYPE A          |      | TYPE C          |      | ARROW BOARDS |      | SIGNS    |      | DETOUR   |       |
|                 |               |    |         |      |          |       |                 |      |                 |      |                 |      |              |      |          |      | SIGNS    |       |
| CATEGORY        | STATION       | TO | STATION | DAYS | DRUM     | DAYS  | BARRICADE       | DAYS | EACH            | DAYS | EACH            | DAYS | EACH         | DAYS | EACH     | DAYS | EACH     | DAYS  |
| HOAN BRIDGE     |               |    |         |      |          |       |                 |      |                 |      |                 |      |              |      |          |      |          |       |
| 1000            | UNDISTRIBUTED |    |         |      | 76       | 1,999 | 2               | 53   | 2               | 53   | 22              | 595  | 3            | 81   | 12       | 318  | 67       | 1,005 |
| PROJECT TOTAL   |               |    |         |      | 1,999    |       | 53              |      | 53              |      | 595             |      | 81           |      | 318      |      | 1,005    |       |

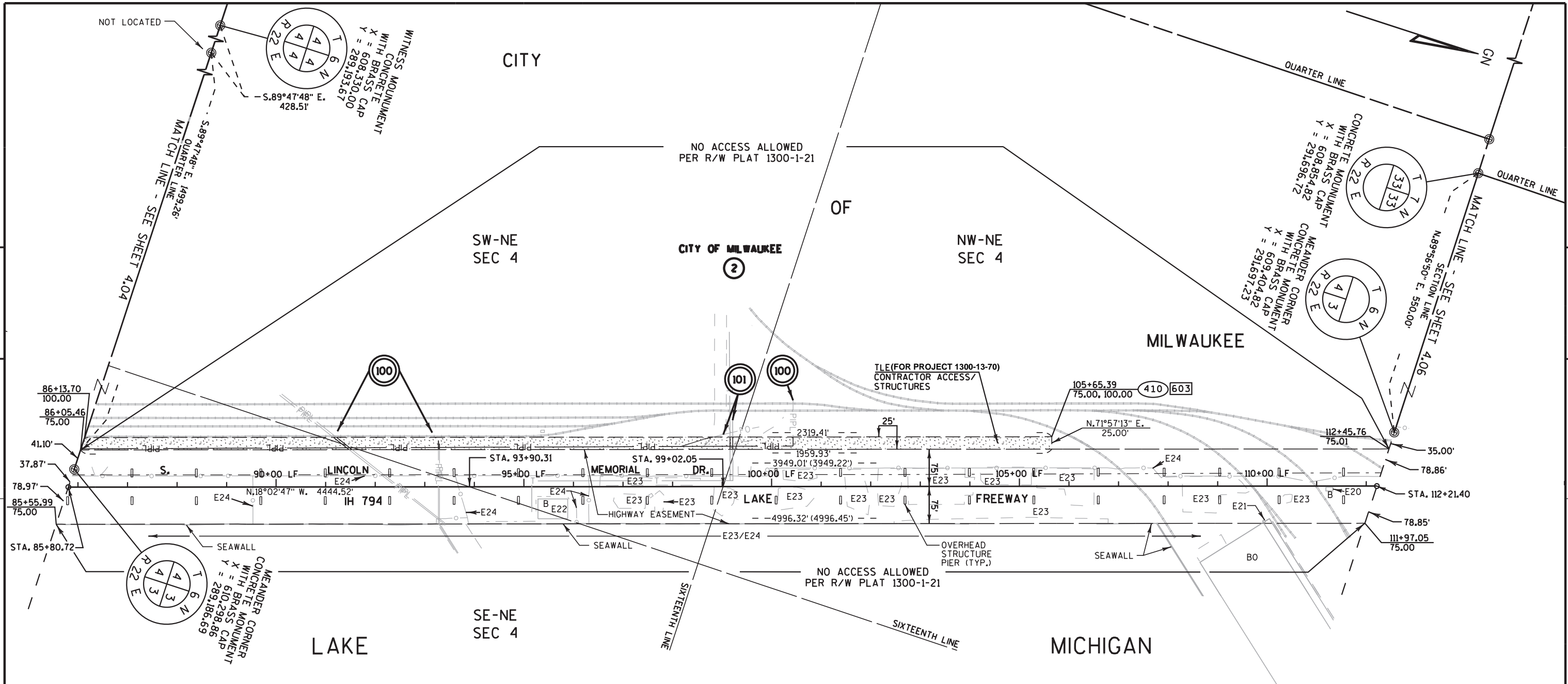
| TRAFFIC CONTROL SIGNS PCMS |               |        |           |  |
|----------------------------|---------------|--------|-----------|--|
|                            |               |        |           | TRAFFIC<br>CONTROL<br>SIGNS PCMS<br>643.1050 |
| CATEGORY                   | STATION       | OFFSET | DIRECTION | DAY  |
| HOAN BRIDGE                |               |        |           |  |
|                            | UNDISTRIBUTED | 36 '   | LT        | 360  |
| PROJECT TOTAL              |               |        |           | 360  |











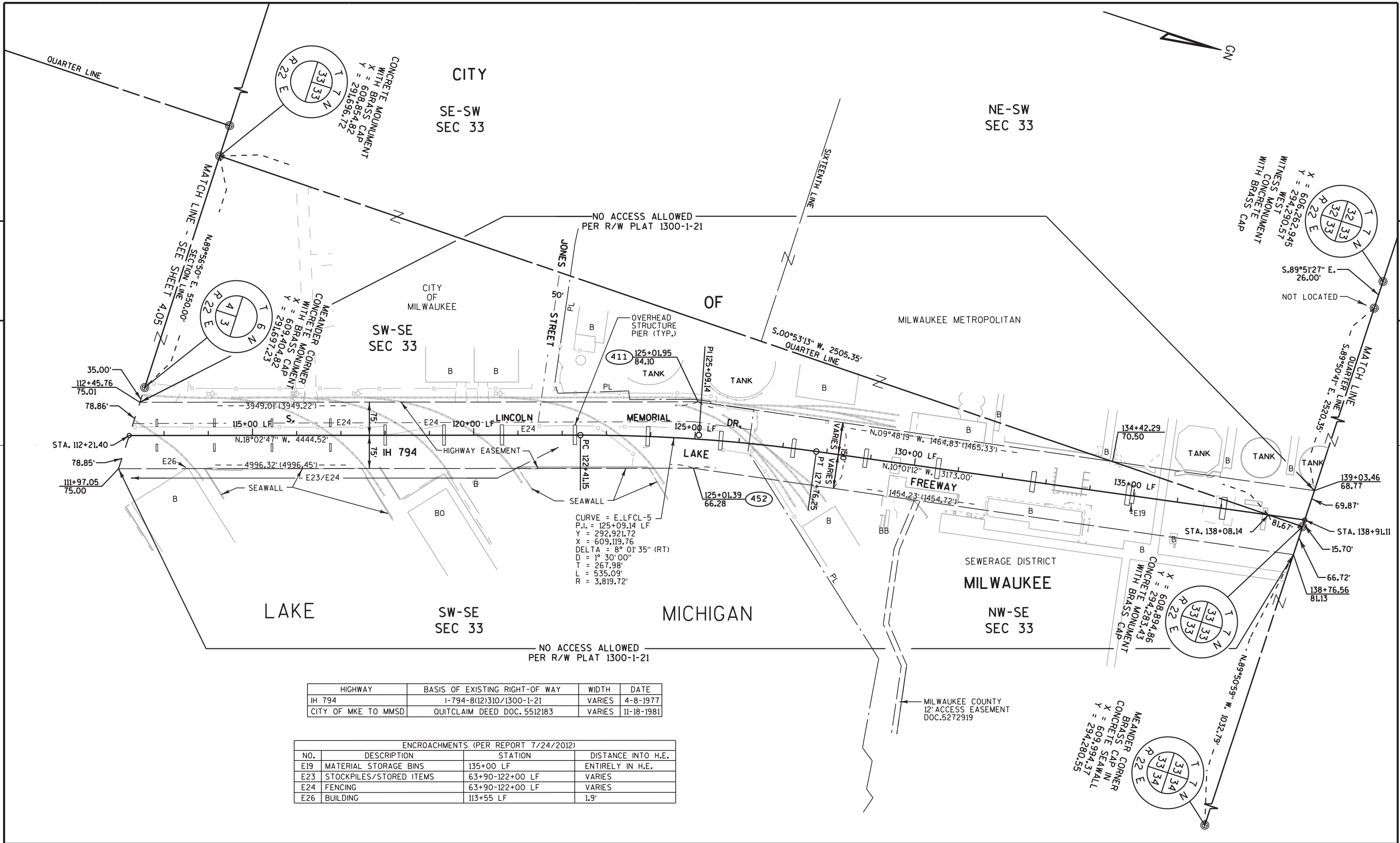


| WIDTH   |                         |                     |  |
|---|-------------------------|---------------------|--|
|  | WEST SHORE PIPELINE CO. | VARIABLES           | DOC. 3897229 DEED 4165 PAGE 79<br>SUPPLEMENTAL AGREEMENT DEC. 13, 1961<br>EXTENSION AND AMENDMENT OF EASEMENT AND LEASE AGREEMENT, APRIL 23, 1984<br>AMENDMENT OF EASEMENT AND LEASE AGREEMENT, APRIL 21, 1988<br>RESOLUTION 872287, MARCH 8, 1988<br>EXTENSION AND AMENDMENT OF EASEMENT AND LEASE AGREEMENT, MARCH 11, 2008<br>RESOLUTION 071306, FEBRUARY 18, 2008<br>THIRD AMENDMENT TO EASEMENT AND LEASE AGREEMENT, JULY 10, 2012<br>RESOLUTION 111441, MARCH 28, 2012 |
|  | AT&T WISCONSIN          | GENERAL DESCRIPTION | DOC. 5275457<br>REEL 1168 IMAGE 847  |
|   |                         |                     | PAR. 2   |
|   |                         |                     | PAR. 2   |

| HIGHWAY | BASIS OF EXISTING RIGHT-OF WAY | WIDTH  | DATE     |
|---------|--------------------------------|--------|----------|
| IH 794  | I-794-8(12)310/1300-1-21       | VARIES | 4-8-1977 |

| ENCROACHMENTS (PER REPORT 7/24/2012) |                                 |                 |                    |
|--------------------------------------|---------------------------------|-----------------|--------------------|
| NO.                                  | DESCRIPTION                     | STATION         | DISTANCE INTO H.E. |
| E20                                  | GUARD SHACK                     | 111+50 LF       | ENTIRELY IN H.E.   |
| E21                                  | BLDG/OVERHANG ON PORT WAREHOUSE | 110+05 LF       | 9.2'               |
| E22                                  | STORAGE BUILDING                | 95+60 LF        | ENTIRELY IN H.E.   |
| E23                                  | STOCKPILES/STORED ITEMS         | 63+90-122+00 LF | VARIES             |
| E24                                  | FENCING                         | 63+90-122+00 LF | VARIES             |

|                                       |                 |                          |                   |  |                 |   |
|---------------------------------------|-----------------|--------------------------|-------------------|--|-----------------|---|
| REVISION DATE<br>10-18-12<br>02-28-13 | DATE 09/25/2012 | 0 100 200<br>SCALE, FEET | HWY: IH 794       | STATE R/W PROJECT NUMBER 1300-13-20    | PLAT SHEET 4.05 | E |
|                                       |                 |                          | COUNTY: MILWAUKEE | CONSTRUCTION PROJECT NUMBER 1300-13-71 | PS&E SHEET      |   |



| HIGHWAY             | BASIS OF EXISTING RIGHT-OF WAY | WIDTH  | DATE       |
|---------------------|--------------------------------|--------|------------|
| IH 794              | I-794-8(12)310/1300-1-21       | VARIES | 4-8-1977   |
| CITY OF MKE TO MMSD | QUITCLAIM DEED DOC. 5512183    | VARIES | 11-18-1981 |

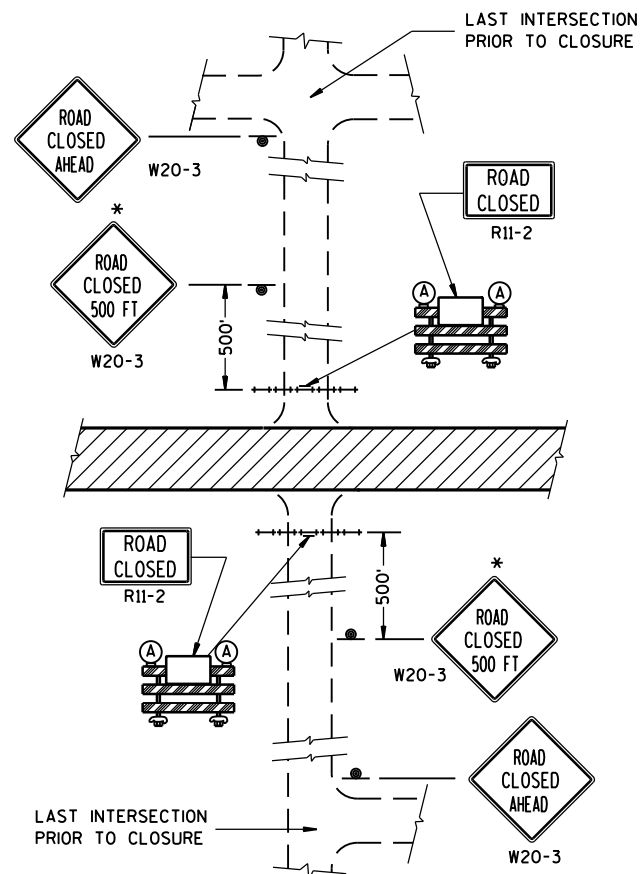
| ENCROACHMENTS (PER REPORT 7/24/2012) |                         |                 |                    |
|--------------------------------------|-------------------------|-----------------|--------------------|
| NO.                                  | DESCRIPTION             | STATION         | DISTANCE INTO H.E. |
| E19                                  | MATERIAL STORAGE BINS   | 135+00 LF       | ENTIRELY IN H.E.   |
| E23                                  | STOCKPILES/STORED ITEMS | 63+90-122+00 LF | VARIES             |
| E24                                  | FENCING                 | 63+90-122+00 LF | VARIES             |
| E26                                  | BUILDING                | 113+55 LF       | 1.9'               |

CURVE = E.LFCL-5  
P.I. = 125+09.14 LF  
Y = 292,921.72  
X = 609,119.76  
DELTA = 8° 01' 35" (RT)  
D = 1° 30' 00"  
T = 267.98'  
L = 535.09'  
R = 3,819.72'

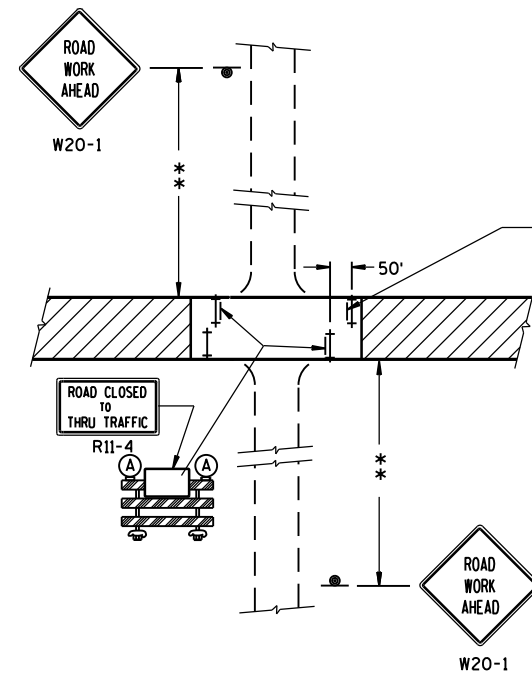
|                                       |                 |                          |                   |  |                 |
|---------------------------------------|-----------------|--------------------------|-------------------|--|-----------------|
| REVISION DATE<br>10-18-12<br>02-28-13 | DATE 09/25/2012 | 0 100 200<br>SCALE, FEET | HWY: IH 794       | STATE R/W PROJECT NUMBER 1300-13-20    | PLAT SHEET 4.06 |
|                                       |                 |                          | COUNTY: MILWAUKEE | CONSTRUCTION PROJECT NUMBER 1300-13-71 | PS&E SHEET      |

Standard Detail Drawing List

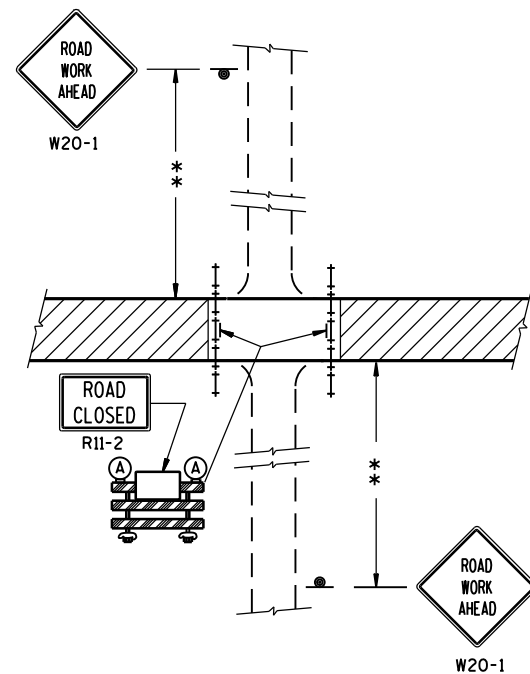
|           |  |
|-----------|--|
| 15C03-02  | BARRICADES AND SIGNS FOR SIDEROAD CLOSURES                                       |
| 15D03-02  | TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M. P. H. WITH BARRIER      |
| 15D12-05A | TRAFFIC CONTROL, LANE CLOSURE  |
| 15D15-01  | TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE                      |
| 15D16-02  | TRAFFIC CONTROL, EXIT RAMP CLOSURE   |
| 15D20-03  | TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY                     |
| 15D27-02  | TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH |
| 15D28-02  | TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY             |



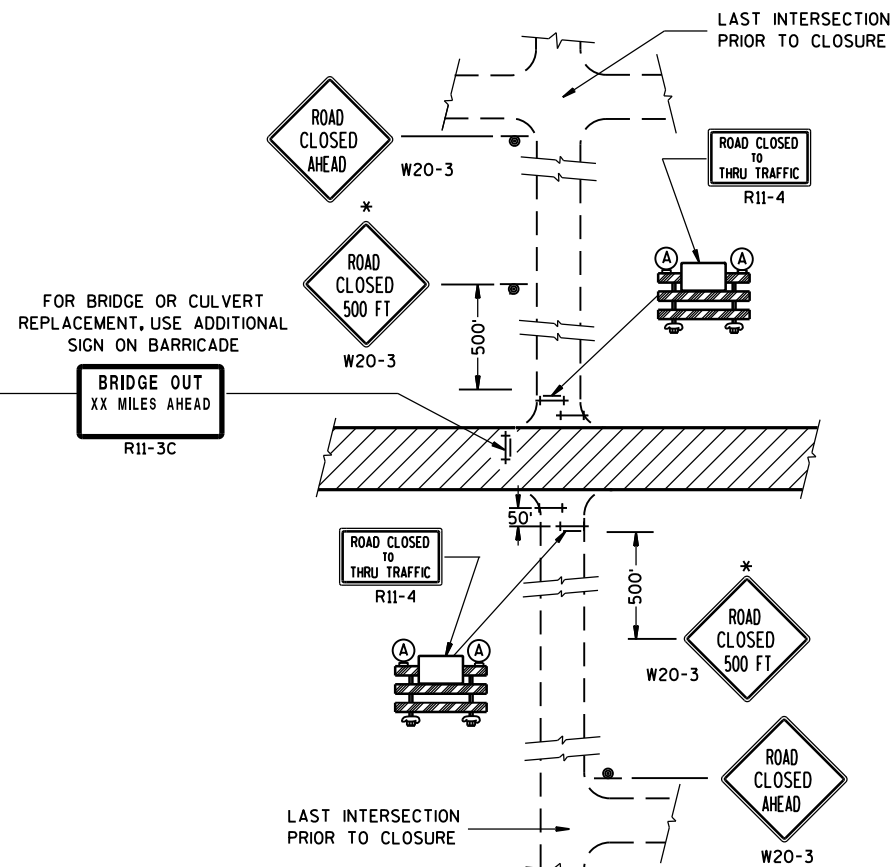
**DETAIL 1**  
(NO ACCESS TO PROJECT)



**DETAIL 3**  
(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



**DETAIL 2**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
NO ACCESS TO PROJECT).



**DETAIL 4**  
(CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)

## GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-4 AND R11-3 SHALL BE 60" X 30".

\*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

\*\*500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

## LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊢ TYPE III BARRICADE
- ⊢ TYPE III BARRICADE WITH ATTACHED SIGN
- Ⓐ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

## BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

8/2013 /S/ Travis Feltes

DATE STATE TRAFFIC ENGINEER OF DESIGN

FHWA



LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- REMOVING PAVEMENT MARKING
- CONCRETE BARRIER TEMPORARY PRECAST
- DIRECTION OF TRAFFIC
- WORK AREA



W02-52  
36"x24"

INSTALL ON EACH APPROACH AT THE CLOSEST INTERSECTION WITH A STATE OR COUNTY TRUNK HIGHWAY, OR AS DIRECTED BY THE ENGINEER. WIDTH ON SIGN TO BE APPROX. 1 FOOT LESS THAN AVAILABLE WIDTH (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET).



LOCATED 500 FEET IN ADVANCE OF R2-1 SIGN AND 500 FEET BEYOND THE "ROAD WORK 1 MILE" SIGN.



R2-1  
48"x60"  
(BLACK AND WHITE)

IF THE REGULATORY SPEED HAS BEEN REDUCED, A SPEED LIMIT SIGN SHALL BE LOCATED 1500 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP. THERE SHOULD BE A SPEED LIMIT SIGN INCORPORATED A MINIMUM OF EVERY 2 OR 3 MILES.

\* INCLUDE RESUME SPEED LIMIT SIGN A MINIMUM OF 200 FEET (500 FEET DESIRABLE) AFTER END ROAD WORK SIGNS.

GENERAL NOTES

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

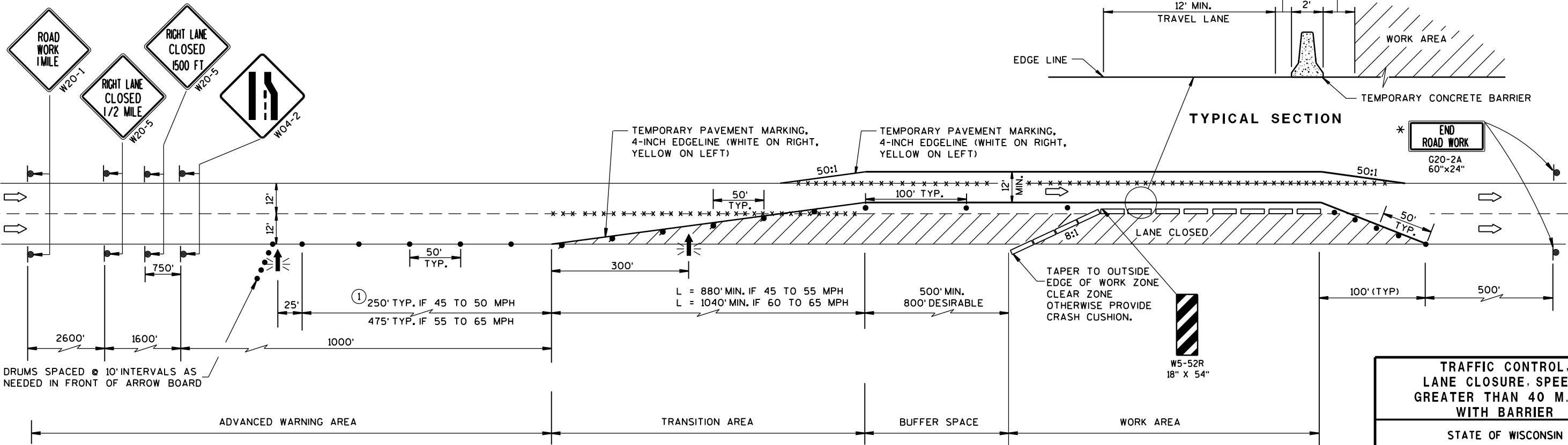
- ① CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUM TAPER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.



DRUMS SPACED @ 10' INTERVALS AS NEEDED IN FRONT OF ARROW BOARD

ADVANCED WARNING AREA

TRANSITION AREA

BUFFER SPACE

WORK AREA

TRAFFIC CONTROL,  
LANE CLOSURE, SPEEDS  
GREATER THAN 40 M.P.H.  
WITH BARRIER

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
8/2013 /S/ Travis Feltes  
DATE STATE TRAFFIC ENGINEER OF DESIGN  
FHWA

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMENENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

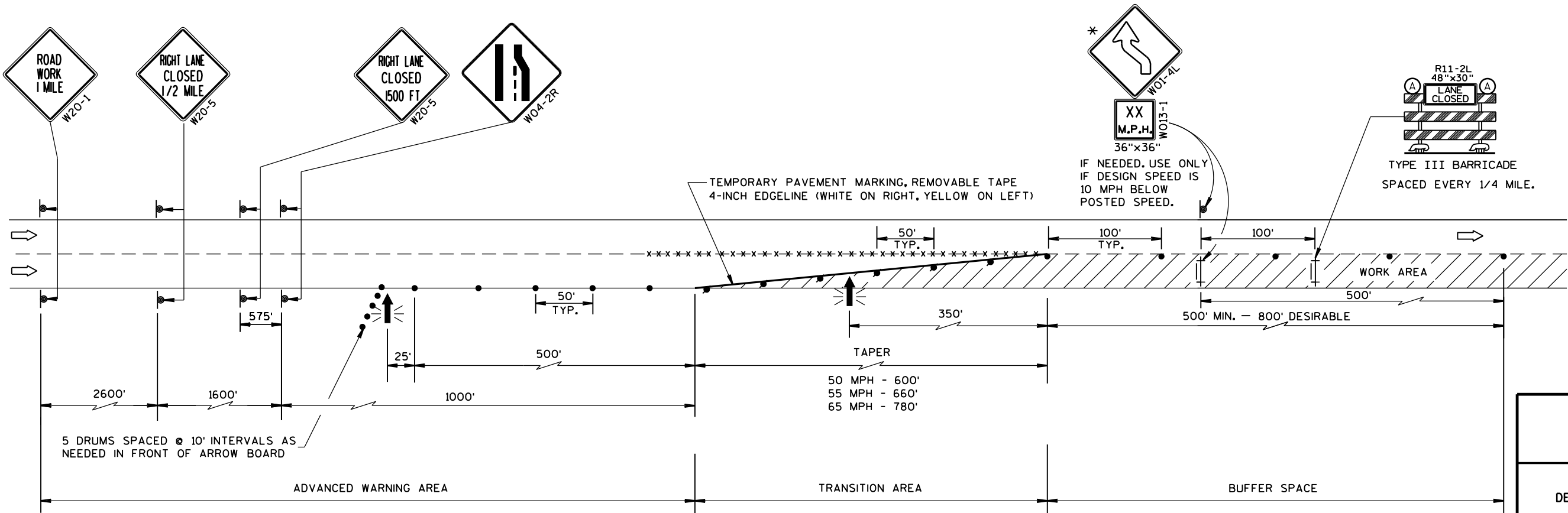
REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR LONGER THAN 4 OR MORE DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

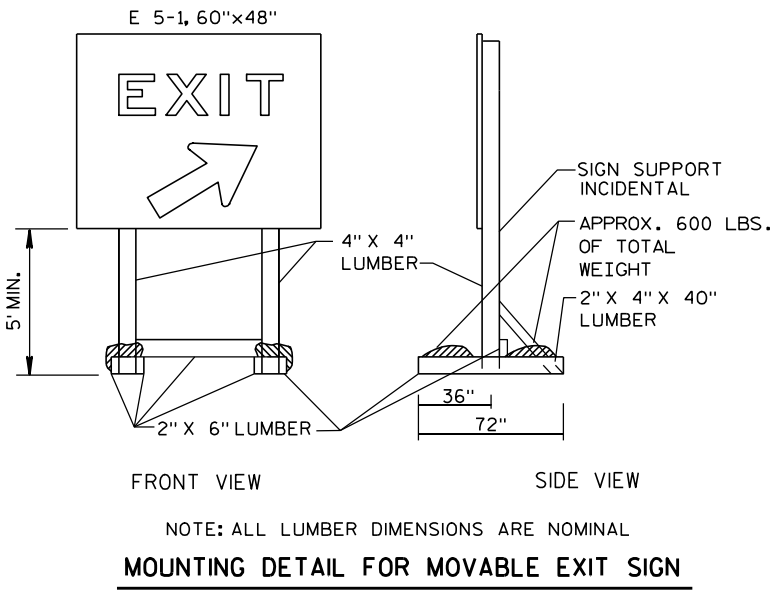
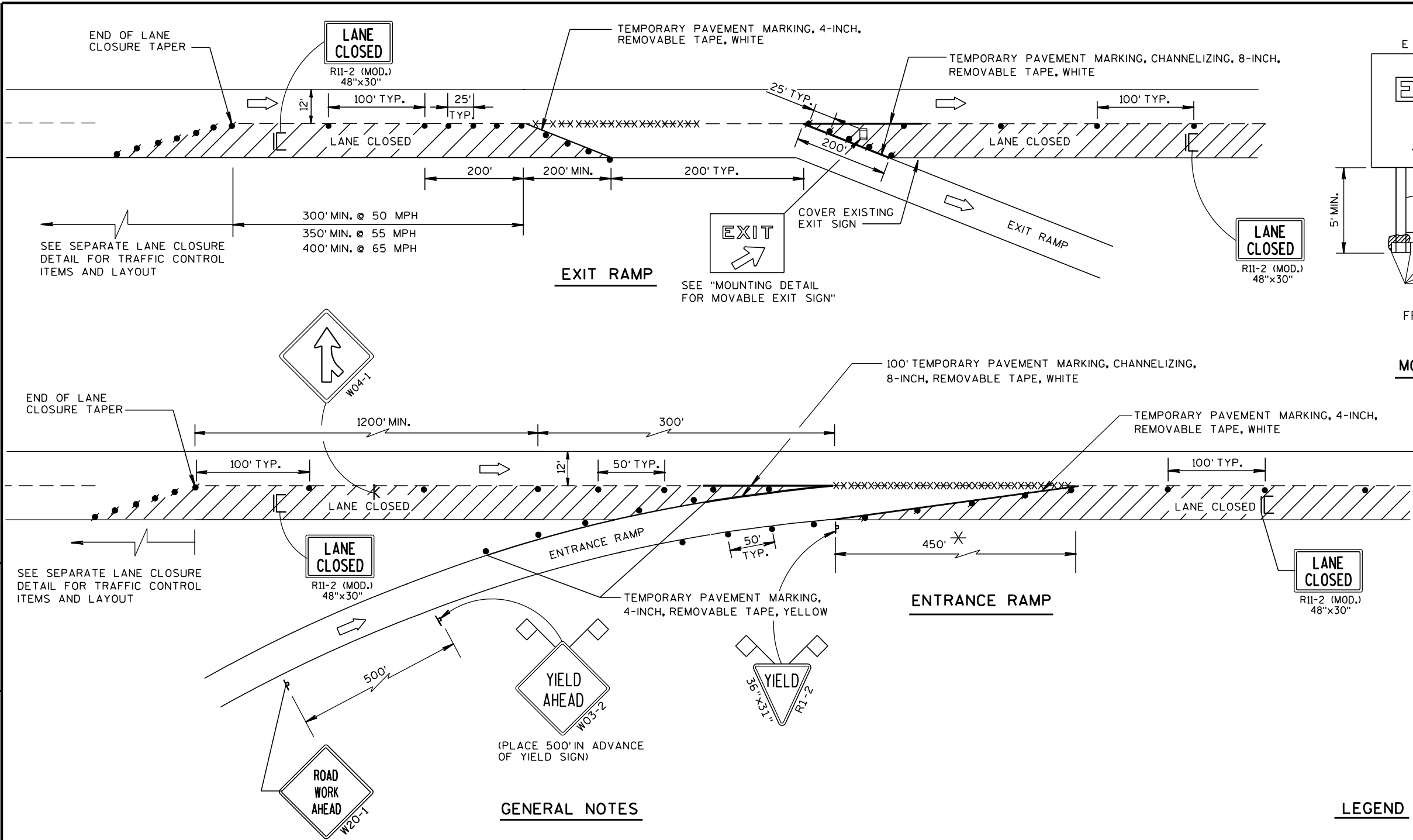
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

\* THE LEFT REVERSE CURVE SIGN (WO1-4L) IS ONLY REQUIRED WHEN THIS DETAIL IS USED IN COMBINATION WITH "SINGLE LANE CROSSOVER" DETAIL.



|  |   |
|--|---|
| TRAFFIC CONTROL,<br>LANE CLOSURE                   |   |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |   |
| APPROVED<br>Feb. 2015<br>DATE                      | /S/ Travis Fettes<br>STATE TRAFFIC ENGINEER OF DESIGN |
| FHWA   |   |



**GENERAL NOTES**

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2 (MOD.) "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 7 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.







\* LENGTH OF OPENING MAY BE REDUCED TO 150 FEET DURING STAGING OF WORK IN IMMEDIATE AREA OF RAMP TAPER.

**LEGEND**

- POST MOUNTED SIGN
- SIGN ON PORTABLE SUPPORT
- TRAFFIC CONTROL, DRUM
- TRAFFIC CONTROL, DRUM WITH WARNING LIGHT, TYPE C (STEADY-BURN)
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- TYPE III BARRICADE (8' EQUIVALENT) WITH SIGN
- FLAGS, 16"x16" MIN., ORANGE
- DIRECTION OF TRAFFIC FLOW

| TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE |  |
|---|--|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION          |  |
| APPROVED<br>5/24/2000<br>DATE                               | /S/ Chester J. Spang<br>CHIEF SIGNS AND MARKING ENGINEER |
| FHWA  |  |

## LEGEND

-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  SIGN ON PERMANENT SUPPORT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC

## GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

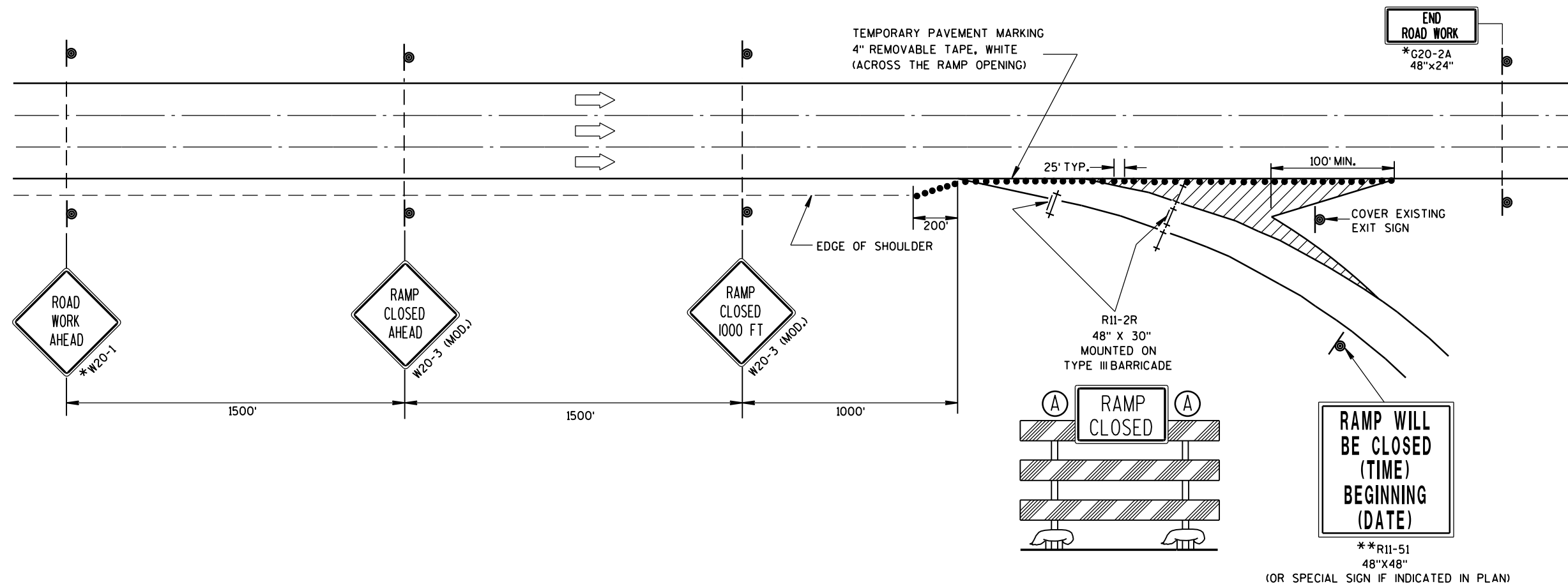
PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF RAMP CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROPOFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN THE WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12-HOUR DURATION.

\*W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

\*\* PLACE "RAMP WILL BE CLOSED" SIGN 10 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



TRAFFIC CONTROL,  
EXIT RAMP CLOSURE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

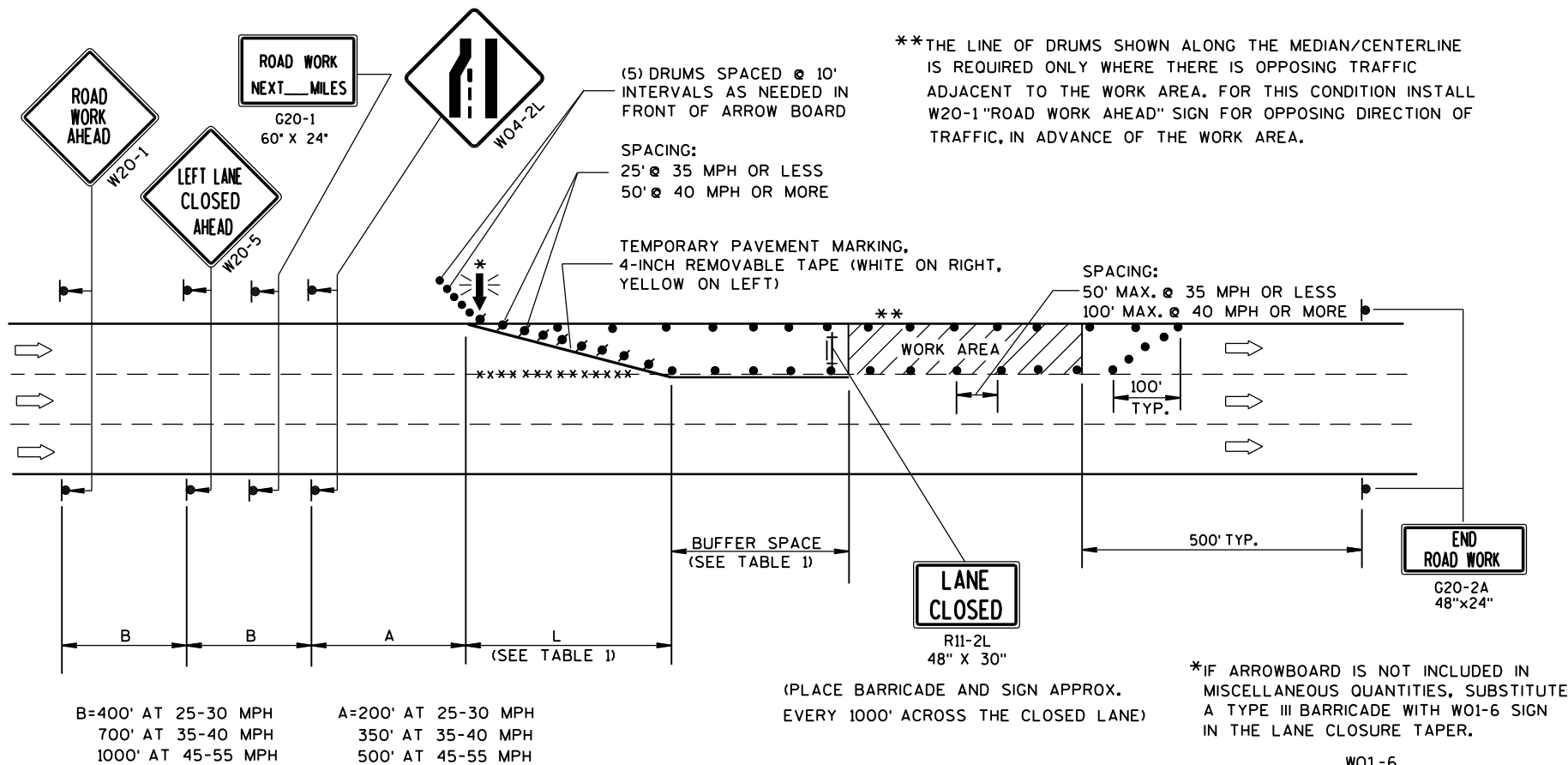
APPROVED

8/2013

DATE

FHWA

/S/ Travis Feltes  
STATE TRAFFIC ENGINEER OF DESIGN



GENERAL NOTES

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W20-1, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TABLE 1  
TAPER AND BUFFER SPACE  
FOR 12' LANE WIDTH

| S  | L    | BUFFER SPACE |
|----|------|--------------|
| 25 | 125' | 55'          |
| 30 | 180' | 85'          |
| 35 | 245' | 120'         |
| 40 | 320' | 170'         |
| 45 | 540' | 220'         |
| 50 | 600' | 280'         |
| 55 | 660' | 335'         |

FOR LANE WIDTH OTHER THAN 12':  
L = WS AT 45 MPH OR GREATER  
L =  $\frac{WS^2}{60}$  AT 40 MPH OR LESS  
L = TAPER LENGTH IN FEET  
S = NON-CONSTRUCTION SPEED LIMIT (MPH)  
W = WIDTH OF LANE CLOSURE

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

|  |                                  |
|--|----------------------------------|
| TRAFFIC CONTROL,<br>SINGLE LANE CLOSURE,<br>NON-FREEWAY/EXPRESSWAY |                                  |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION                 |                                  |
| APPROVED<br>Feb. 2015  | /S/ Travis Feltes                |
| DATE   | STATE TRAFFIC ENGINEER OF DESIGN |
| FHWA   |                                  |

LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ⚡➡ FLASHING ARROW BOARD
- ▨ WORK AREA

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

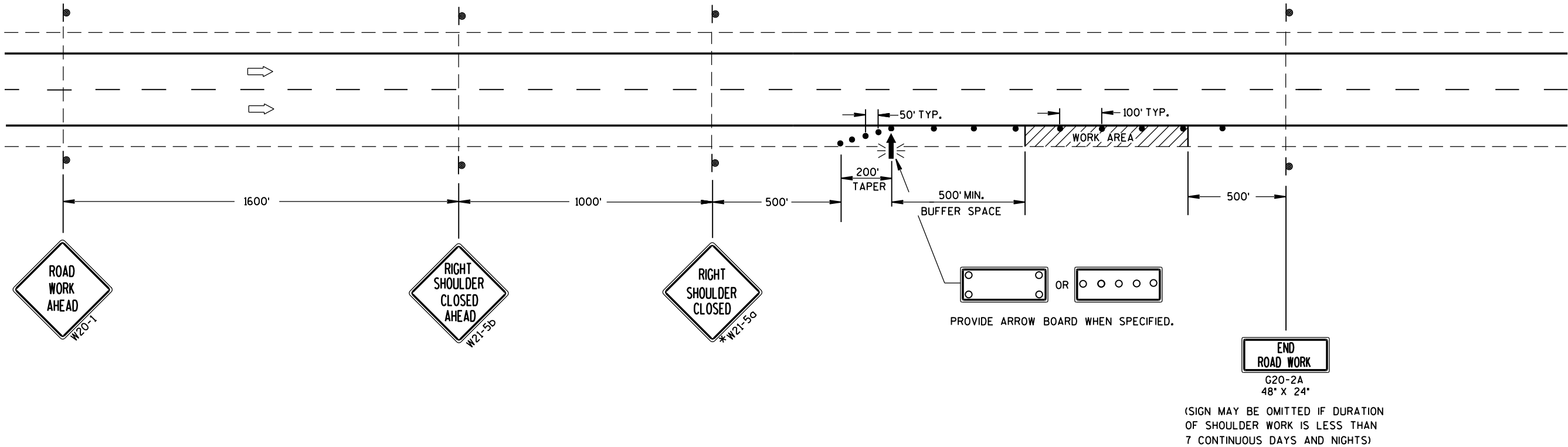
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

\*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-5a SIGN MAY BE OMITTED.



TRAFFIC CONTROL  
SHOULDER CLOSURE ON DIVIDED  
ROADWAY, SPEEDS GREATER  
THAN 40 MPH

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
8/2013 /S/ Travis Feltz  
DATE STATE TRAFFIC ENGINEER OF DESIGN  
FHWA



GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

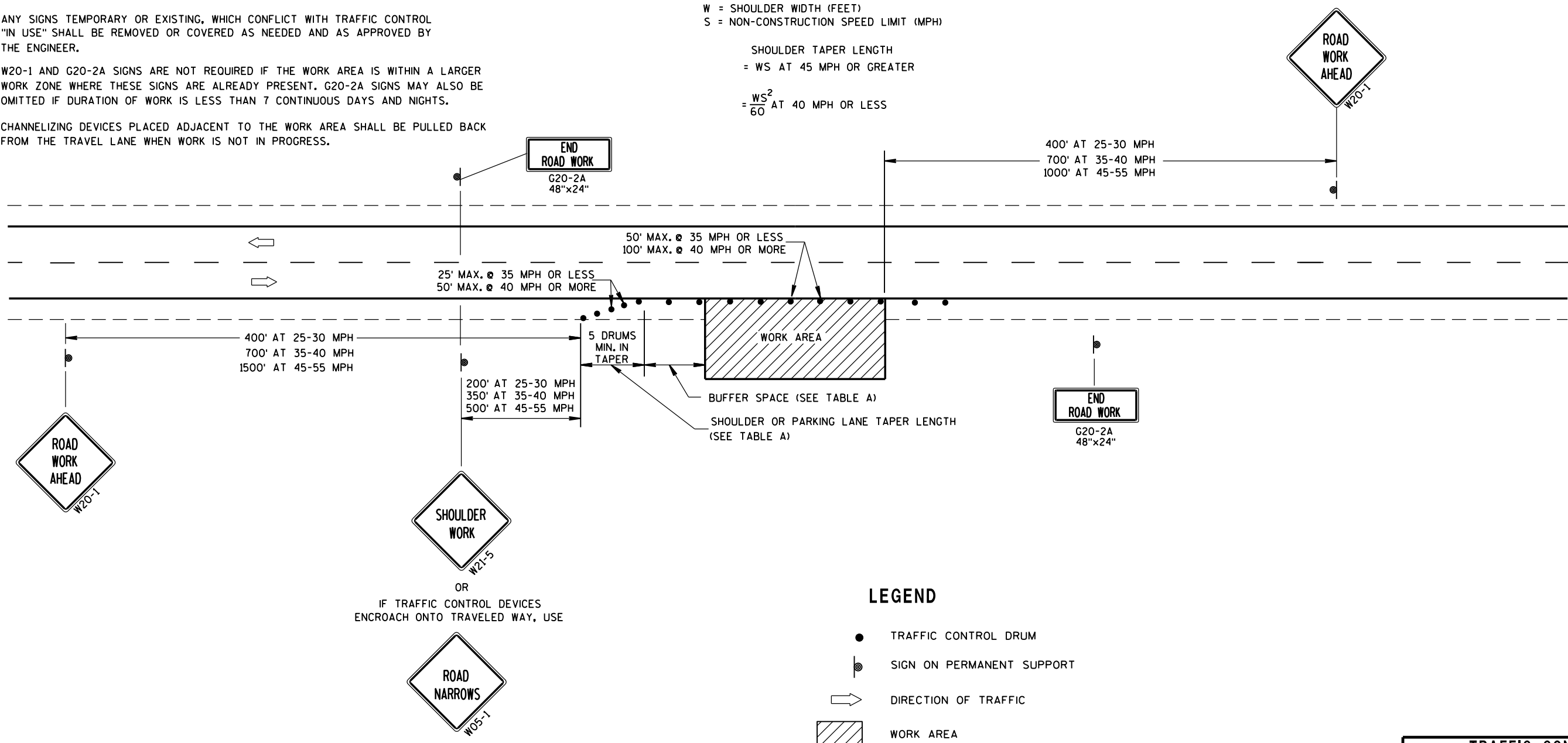
CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

TABLE A

| SHOULDER TAPER LENGTH (FEET) |    |     |     |     | BUFFER SPACE (FEET) |
|------------------------------|----|-----|-----|-----|---------------------|
| S \ W                        | 4  | 6   | 8   | 10  |                     |
| 30                           | 20 | 30  | 40  | 50  | 85                  |
| 35                           | 30 | 45  | 55  | 70  | 120                 |
| 40                           | 40 | 55  | 75  | 90  | 170                 |
| 45                           | 60 | 90  | 120 | 150 | 220                 |
| 50                           | 70 | 100 | 135 | 170 | 280                 |
| 55                           | 75 | 110 | 150 | 185 | 335                 |

W = SHOULDER WIDTH (FEET)  
S = NON-CONSTRUCTION SPEED LIMIT (MPH)

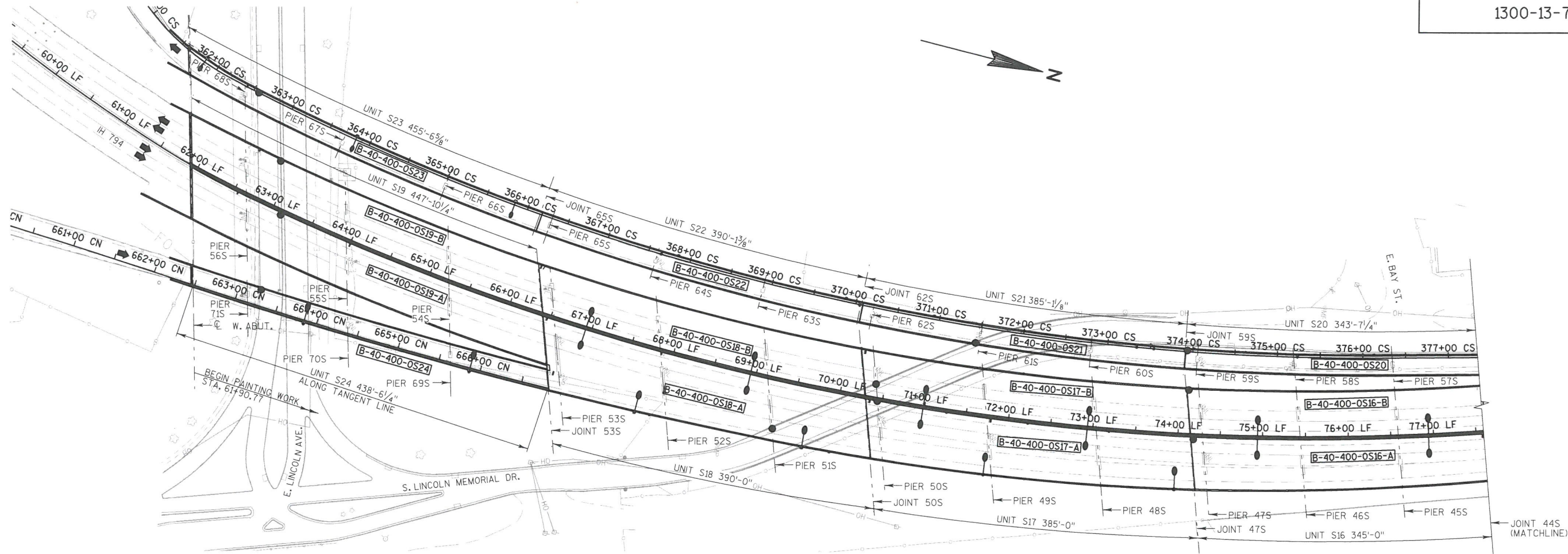
SHOULDER TAPER LENGTH  
= WS AT 45 MPH OR GREATER  
  
=  $\frac{WS^2}{60}$  AT 40 MPH OR LESS



LEGEND

- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ▨ WORK AREA

|   |   |
|---|---|
| TRAFFIC CONTROL,<br>WORK ON SHOULDER OR<br>PARKING LANE,<br>UNDIVIDED ROADWAY |   |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION                            |   |
| APPROVED<br>8/2013<br>DATE  | /S/ Travis Feltes<br>STATE TRAFFIC ENGINEER OF DESIGN |
| FHWA  |   |



PLAN

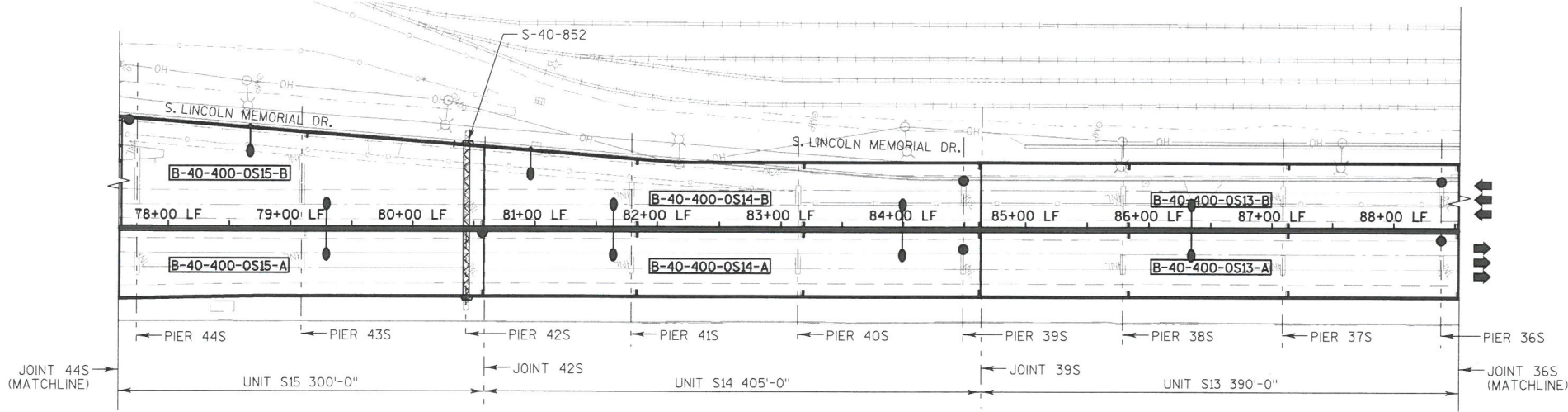
PAINTING  
UNITS S16-A/B, S17-A/B, S18-A/B, S19-A/B, S20, S21, S22, S23 & S24

LEGEND

- POINT OF MINIMUM VERTICAL CLEARANCE
- LIGHT STANDARD

LIST OF DRAWINGS

1. GENERAL PLAN 1
2. GENERAL PLAN 2
3. ELEVATIONS 1
4. ELEVATIONS 2
5. ELEVATIONS 3
6. ELEVATIONS 4
7. ELEVATIONS 5
8. ELEVATIONS 6
9. ELEVATIONS 7
10. ELEVATIONS, GENERAL NOTES & QUANTITIES
11. TYPICAL SECTIONS 1
12. TYPICAL SECTIONS 2
13. TYPICAL SECTIONS 3
14. TYPICAL SECTIONS 4
15. TYPICAL SECTIONS 5



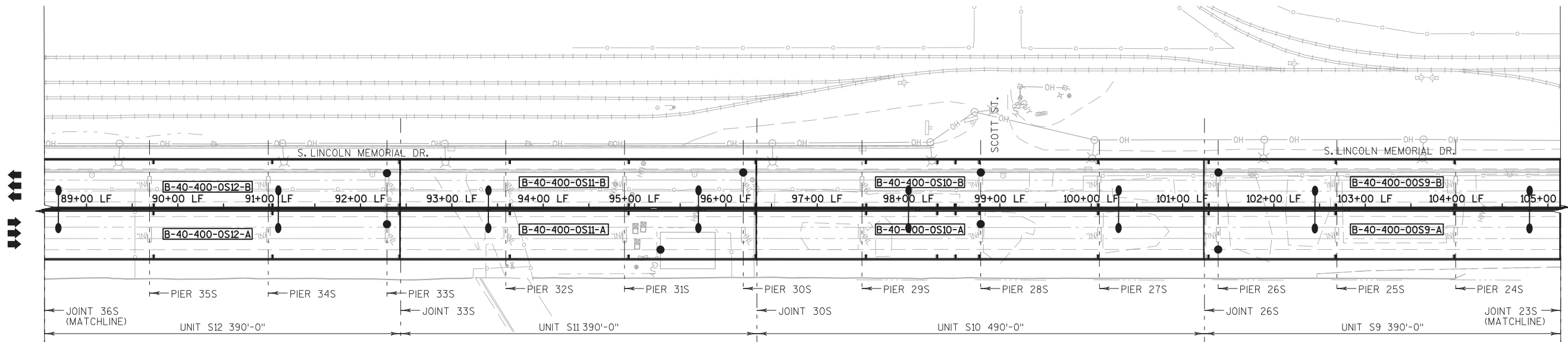
PLAN

PAINTING  
UNITS S13-A/B, S14-A/B & S15-A/B



STRUCTURE DESIGN CONTACTS  
BUREAU OF STRUCTURES:  
WILLIAM DREHER P.E., (608) 266-8489  
  
SE REGION BRIDGE MAINTENANCE:  
JOHN BOLKA P.E., (262) 548-6711  
  
CONSULTANT:  
CHAD HALVERSON P.E., (608) 821-8703

|  |                              |   |                 |
|--|------------------------------|---|-----------------|
| NO.  | DATE                         | REVISION  | BY              |
| <b>Baker</b>                                       |                              | MICHAEL BAKER JR., INC.<br>7633 GANSER WAY, SUITE 206,<br>MADISON, WI 53719 |                 |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |                              |   |                 |
| ACCEPTED   | <i>William C. Dreher</i> SDR |   | <b>08/20/15</b> |
| CHIEF STRUCTURES DESIGN ENGINEER                   |                              |   | DATE            |
| <b>STRUCTURE B-40-400</b>                          |                              |   |                 |
| IH 794 OVER E. LINCOLN AVE. & S. LINCOLN MEM. DR.  |                              |   |                 |
| COUNTY   | MILWAUKEE                    | TOWN/CITY/VILLAGE   | MILWAUKEE       |
| DESIGN SPEC. REHABILITATION N/A                    |                              |   |                 |
| DESIGNED BY  | MSC                          | DESIGN CK'D.  | CDH             |
| DRAWN BY   | MSC                          | PLANS CK'D.   | CDH             |
| GENERAL PLAN 1                                     |                              |   | SHEET 1 OF 15   |



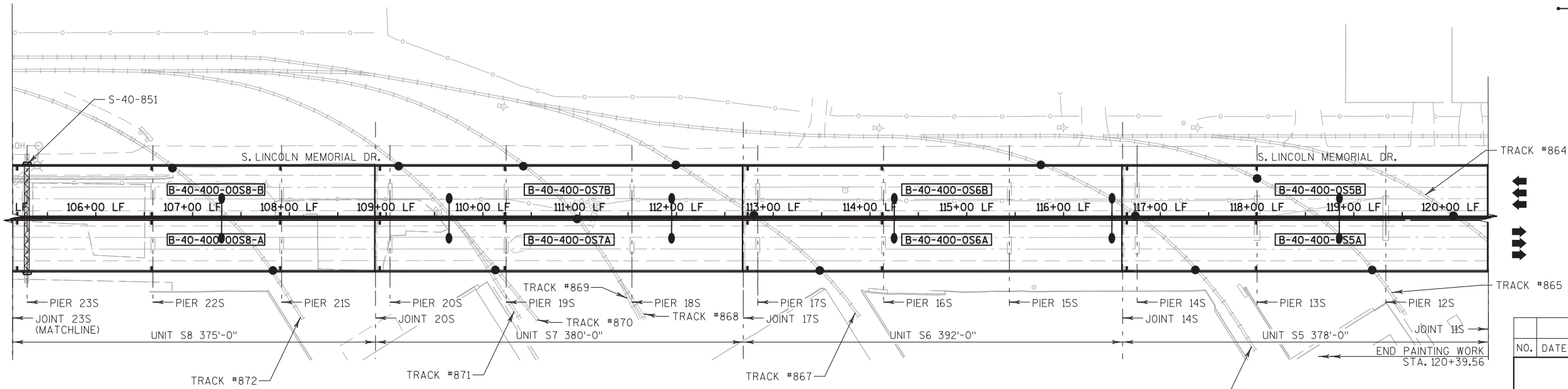
PLAN

PAINTING

UNITS S9-A/B, S10-A/B, S11-A/B, & S12-A/B

LEGEND

- POINT OF MINIMUM VERTICAL CLEARANCE
- LIGHT STANDARD



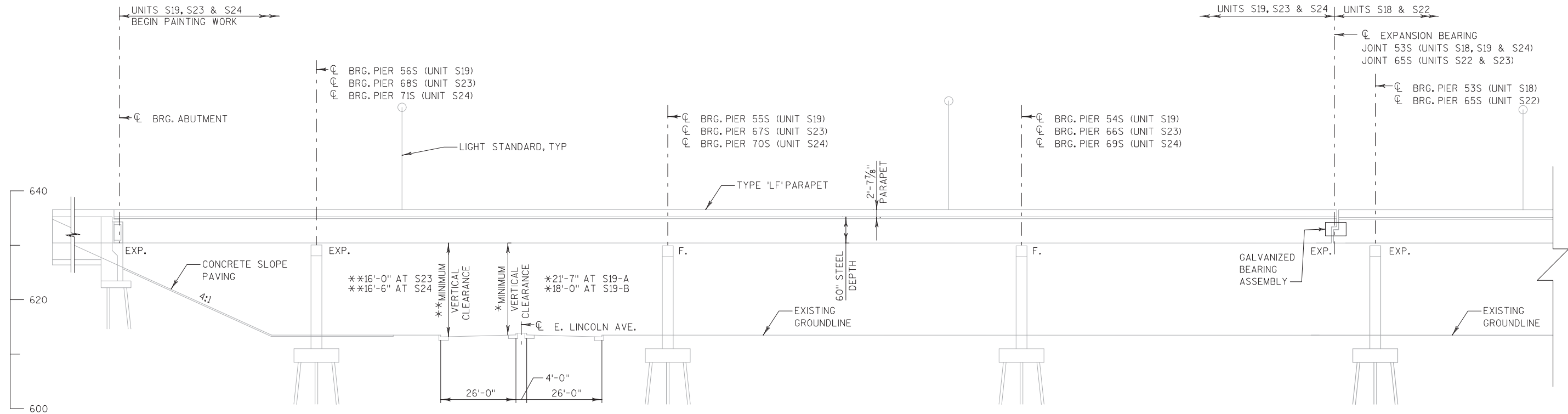
PLAN

PAINTING

UNITS S5A/B, S6A/B, S7A/B, & S8-A/B

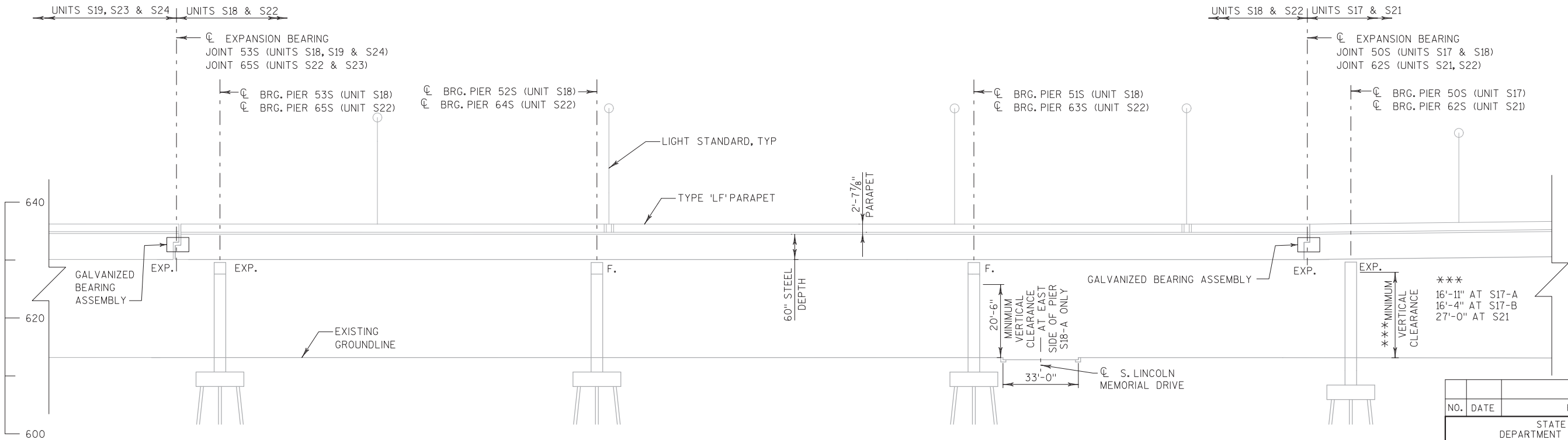
| NO.  | DATE | REVISION        | BY |
|--|------|-----------------|----|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                 |    |
| STRUCTURE B-40-400                                 |      |                 |    |
| DRAWN BY MSC                                       |      | PLANS CK'D. CDH |    |
| GENERAL PLAN 2                                     |      | SHEET 2 OF 15   |    |





ELEVATION - UNITS S19-A/B, S23 & S24

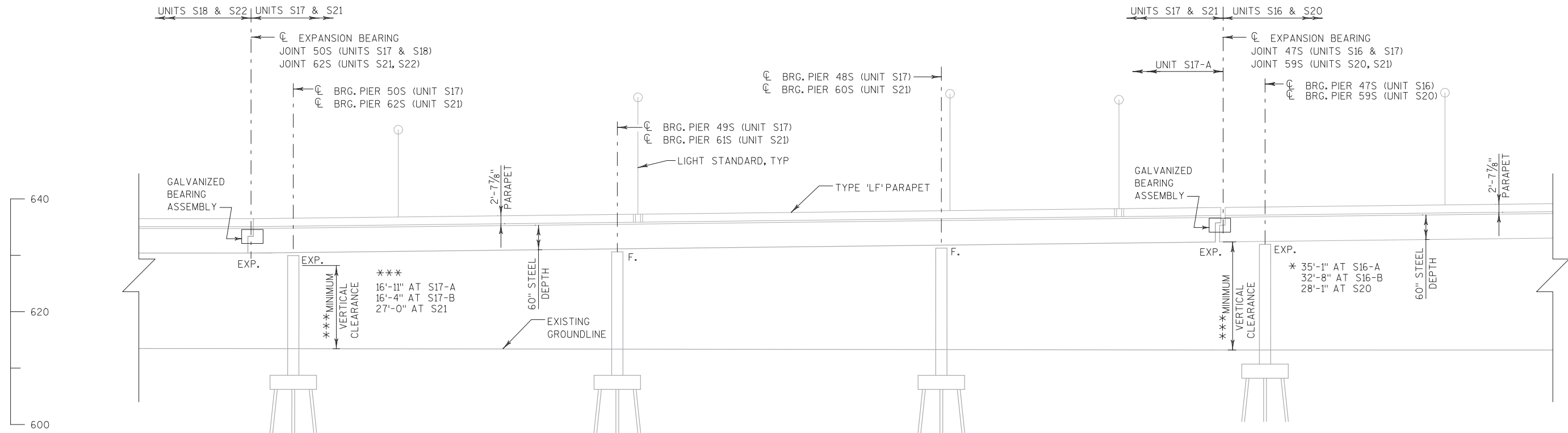
LOOKING WEST  
(S19-A SHOWN, S19-B, S23 & S24 SIMILAR)



ELEVATION - UNITS S18-A/B & S22

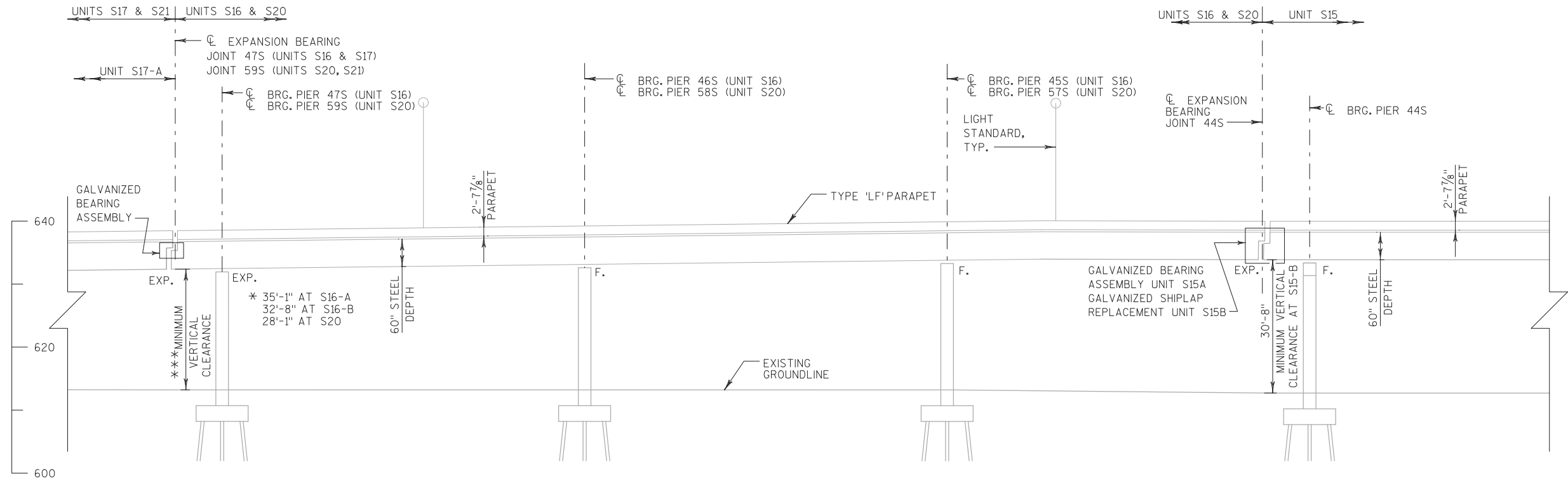
LOOKING WEST  
(S18-A SHOWN, S18-B & S22 SIMILAR)

| NO.  | DATE | REVISION           | BY |
|--|------|--------------------|----|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |    |
| STRUCTURE B-40-400                                 |      |                    |    |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |    |
| ELEVATIONS 1                                       |      | SHEET 3 OF 15      |    |



ELEVATION - UNITS S17-A/B & S21

LOOKING WEST  
(S17-A SHOWN, S17-B & S21 SIMILAR)

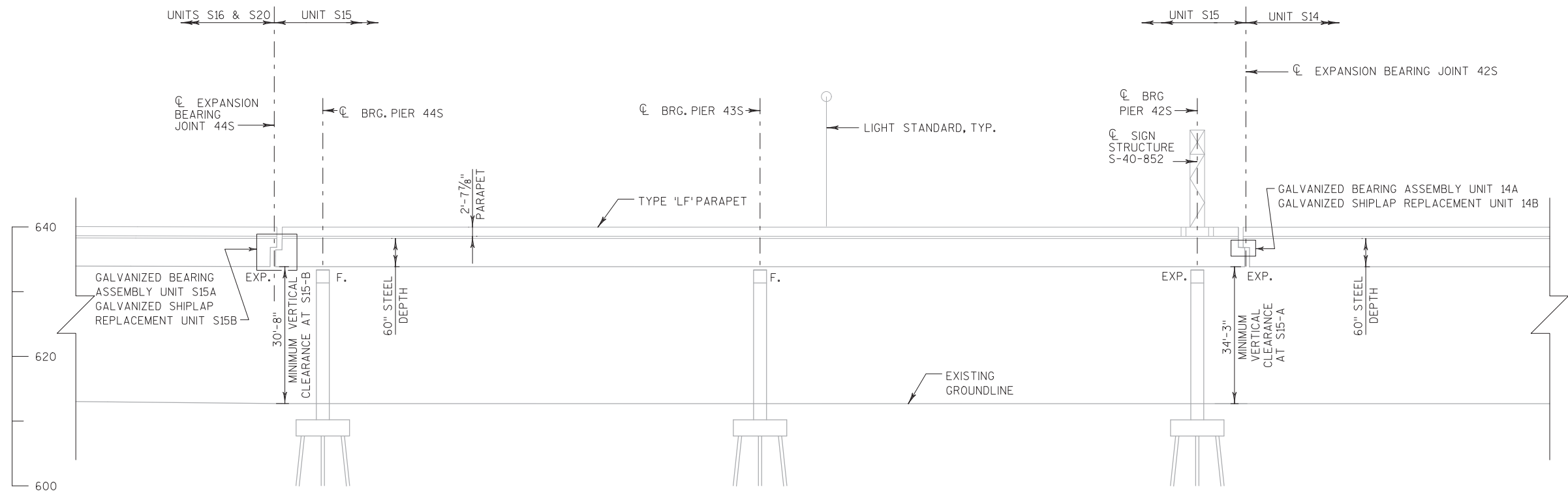
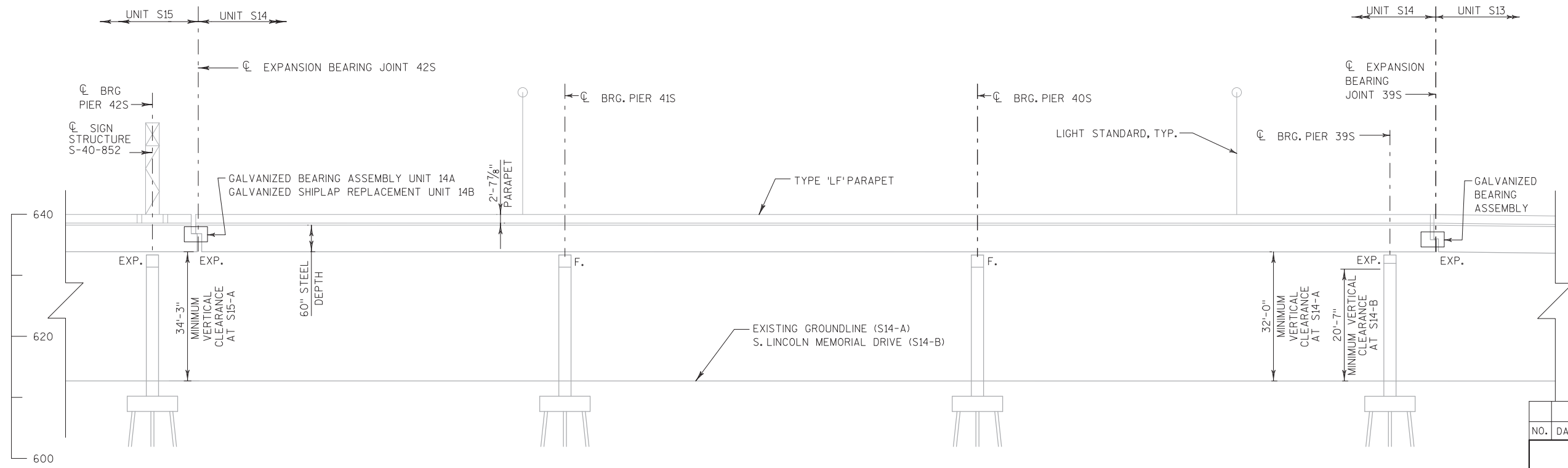


ELEVATION - UNITS S16-A/B & S20

LOOKING WEST  
(S16-A SHOWN, S16-B & S20 SIMILAR)

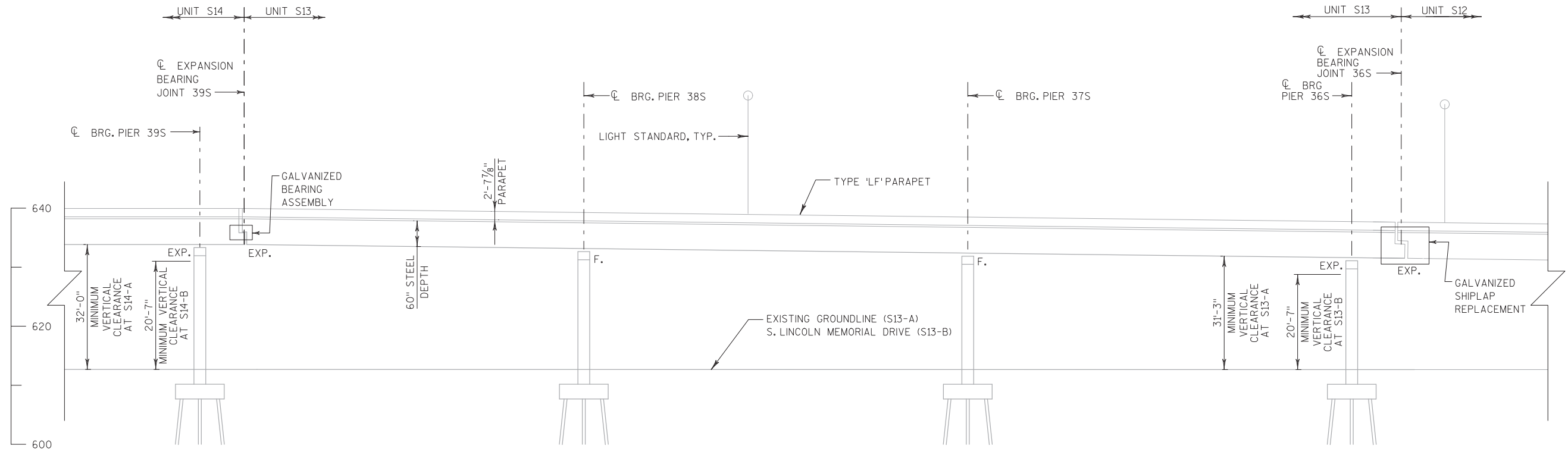
| NO.  | DATE | REVISION           | BY            |
|--|------|--------------------|---------------|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |               |
| STRUCTURE B-40-400                                 |      |                    |               |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |               |
| ELEVATIONS 2                                       |      |                    | SHEET 4 OF 15 |

FILE=  
SCALE =

**ELEVATION - UNIT S15-A/B**LOOKING WEST  
(S15-A SHOWN, S15-B SIMILAR)**ELEVATION - UNIT S14-A/B**LOOKING WEST  
(S14-A SHOWN, S14-B SIMILAR)

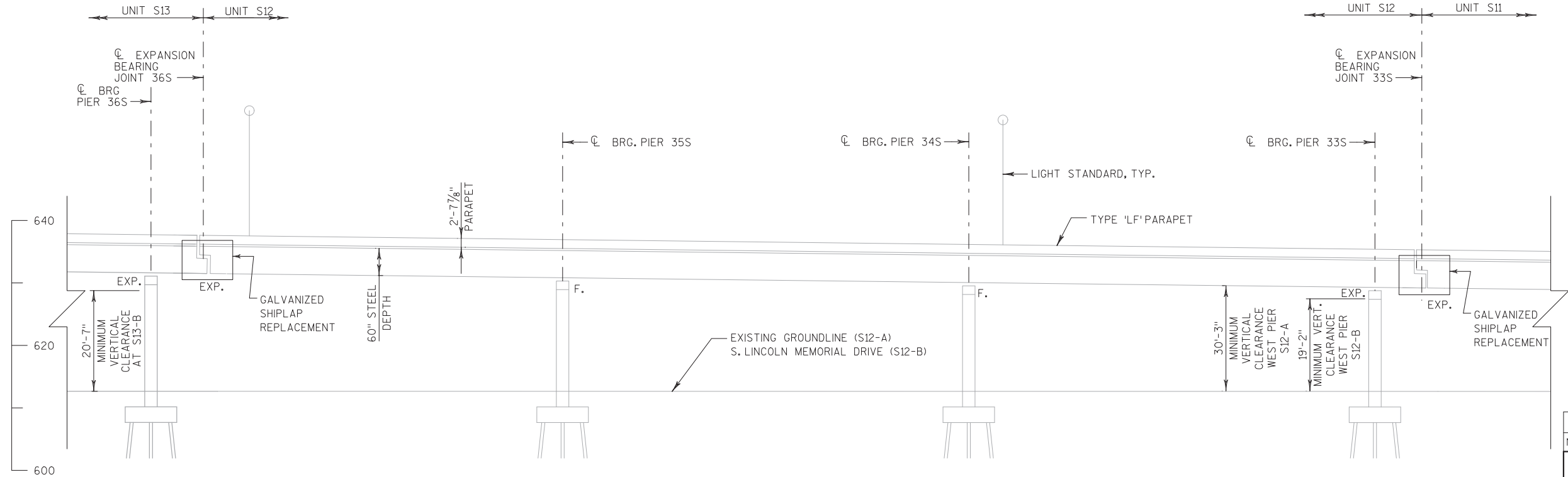
| NO.  | DATE | REVISION           | BY            |
|--|------|--------------------|---------------|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |               |
| STRUCTURE B-40-400                                 |      |                    |               |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |               |
| ELEVATIONS 3                                       |      |                    | SHEET 5 OF 15 |





ELEVATION - UNIT S13-A/B

LOOKING WEST  
(S13-A SHOWN, S13-B SIMILAR)

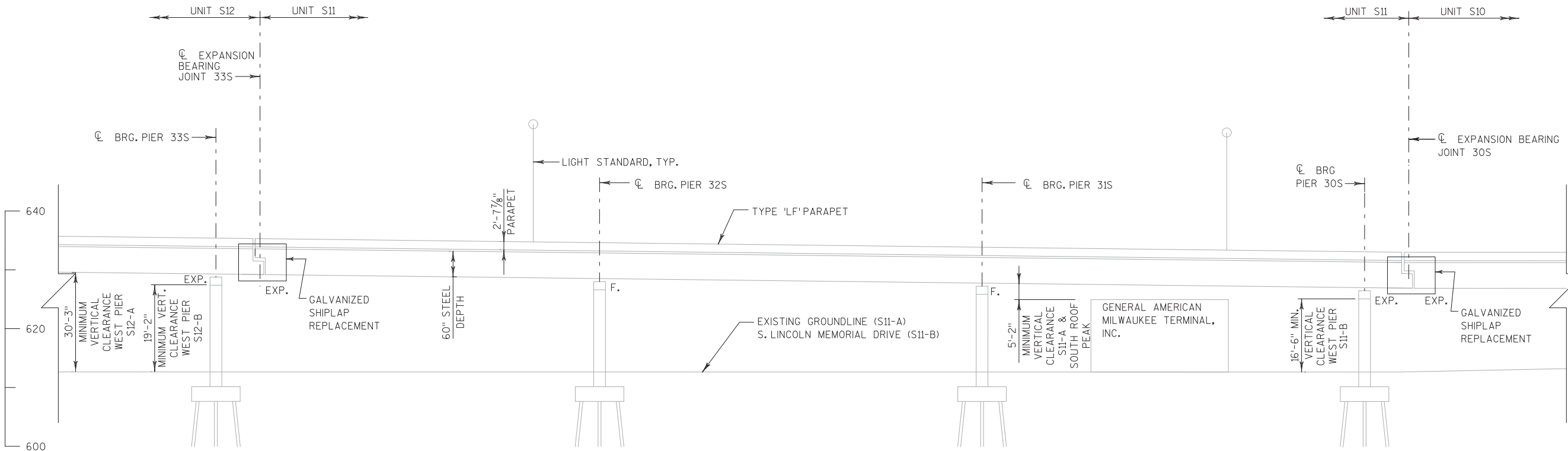


ELEVATION - UNIT S12-A/B

LOOKING WEST  
(S12-A SHOWN, S12-B SIMILAR)

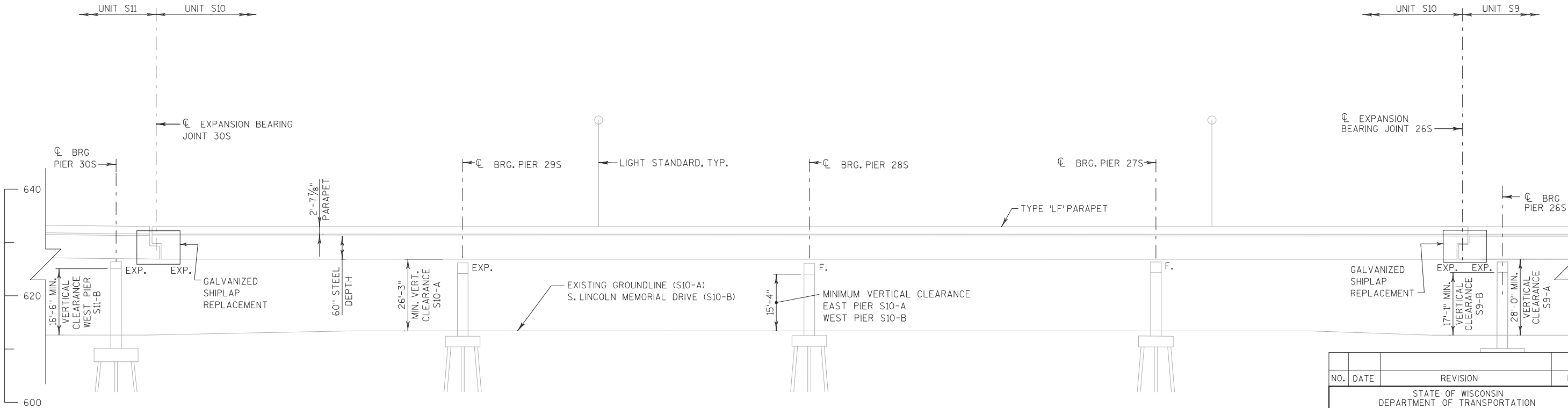
|  |      |                 |               |
|--|------|-----------------|---------------|
| NO.  | DATE | REVISION        | BY            |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                 |               |
| STRUCTURE B-40-400                                 |      |                 |               |
| DRAWN BY MSC                                       |      | PLANS CK'D. CDH |               |
| ELEVATIONS 4                                       |      |                 | SHEET 6 OF 15 |

FILE= SCALE =



ELEVATION - UNIT S11-A/B

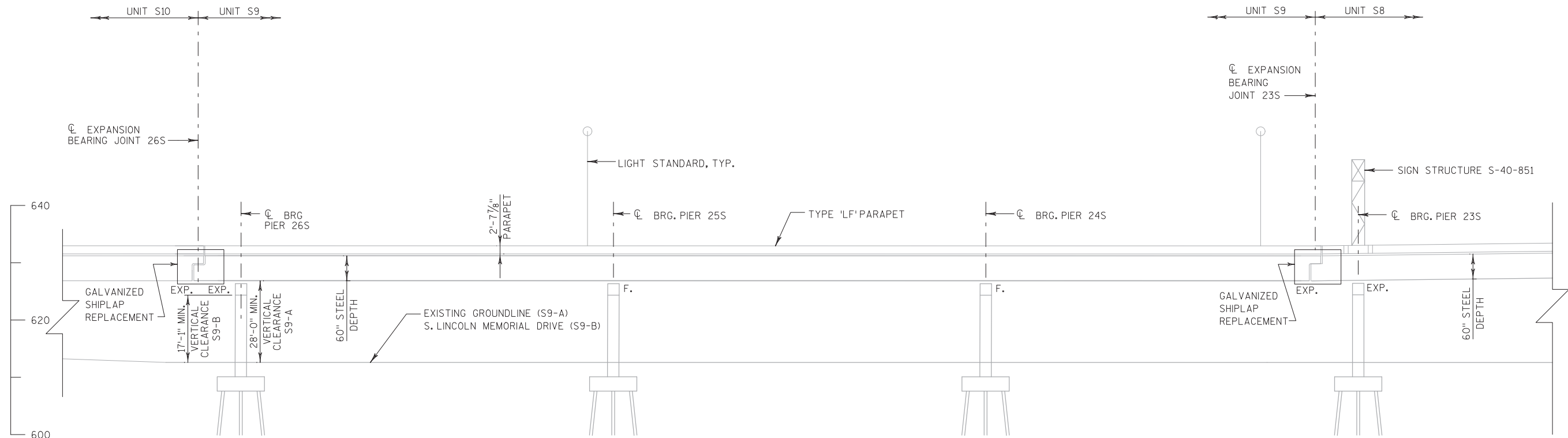
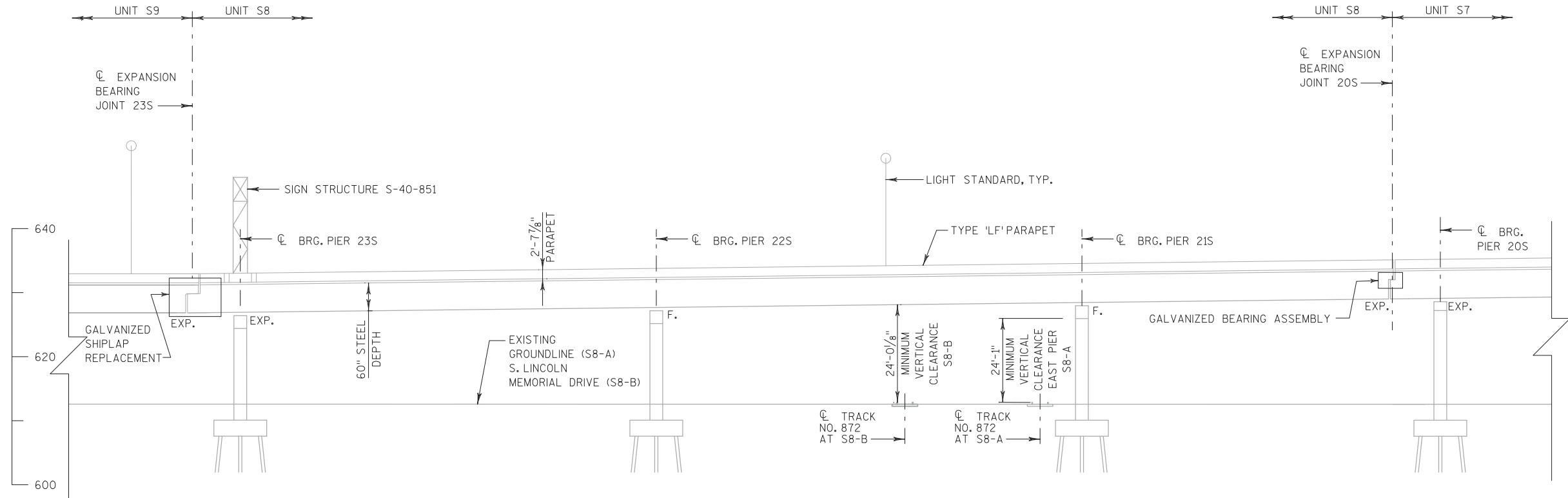
LOOKING WEST  
(S11-A SHOWN, S11-B SIMILAR)



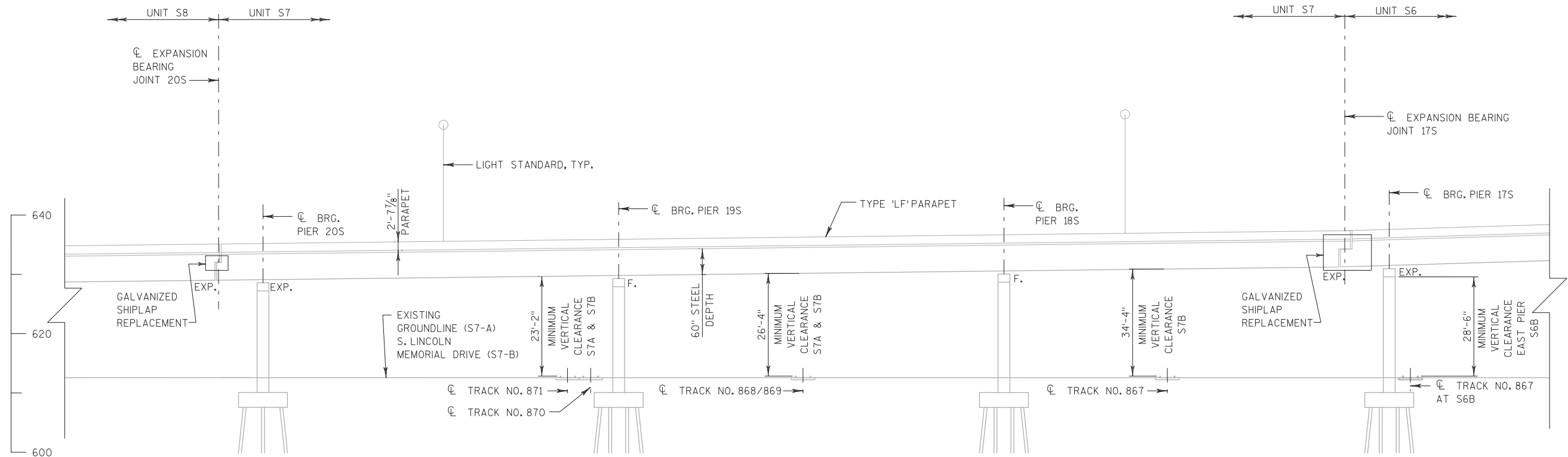
ELEVATION - UNIT S10-A/B

LOOKING WEST  
(S10-A SHOWN, S10-B SIMILAR)

|  |      |                 |                    |
|--|------|-----------------|--------------------|
|  |      |                 |                    |
| NO.  | DATE | REVISION        | BY                 |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                 |                    |
| STRUCTURE B-40-400                                 |      |                 |                    |
|  |      | DRAWN<br>BY MSC | PLANS<br>CK'D. CDH |
| ELEVATIONS 5                                       |      | SHEET 7 OF 15   |                    |
|  |      |                 |                    |

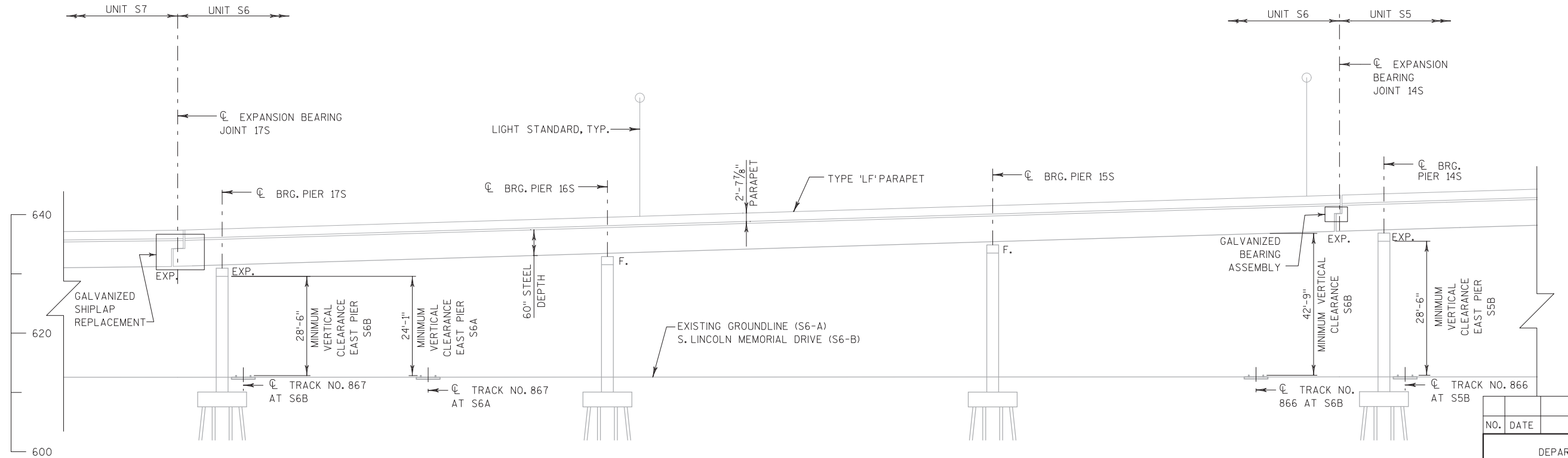
**ELEVATION - UNIT S9-A/B**LOOKING WEST  
(S9-A SHOWN, S9-B SIMILAR)**ELEVATION - UNIT S8-A/B**LOOKING WEST  
(S8-A SHOWN, S8-B SIMILAR)

| NO.  | DATE | REVISION           | BY            |
|--|------|--------------------|---------------|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |               |
| STRUCTURE B-40-400                                 |      |                    |               |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |               |
| ELEVATIONS 6                                       |      |                    | SHEET 8 OF 15 |



ELEVATION - UNIT S7A/B

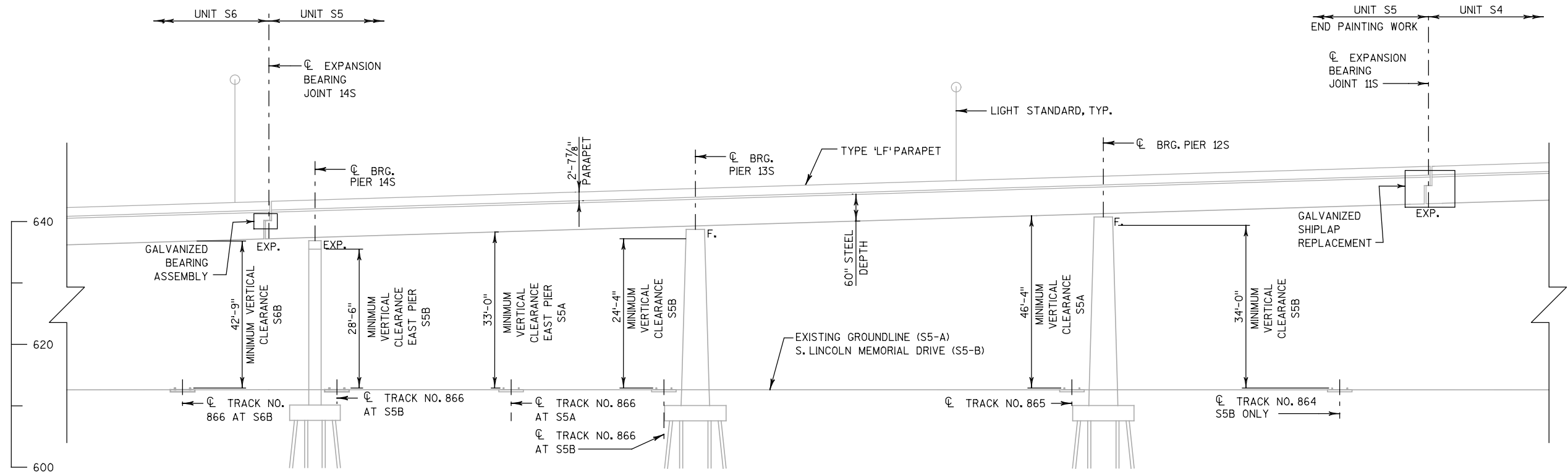
LOOKING WEST  
(S7A SHOWN, S7B SIMILAR)



ELEVATION - UNIT S6A/B

LOOKING WEST  
(S6A SHOWN, S6B SIMILAR)

|  |      |                 |                    |
|--|------|-----------------|--------------------|
|  |      |                 |                    |
| NO.  | DATE | REVISION        | BY                 |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                 |                    |
| STRUCTURE B-40-400                                 |      |                 |                    |
|  |      | DRAWN<br>BY MSC | PLANS<br>CK'D. CDH |
| ELEVATIONS 7                                       |      |                 | SHEET 9 OF 15      |



ELEVATION - UNIT S5A/B  
LOOKING WEST  
(S5A SHOWN, S5B SIMILAR)

GENERAL NOTES

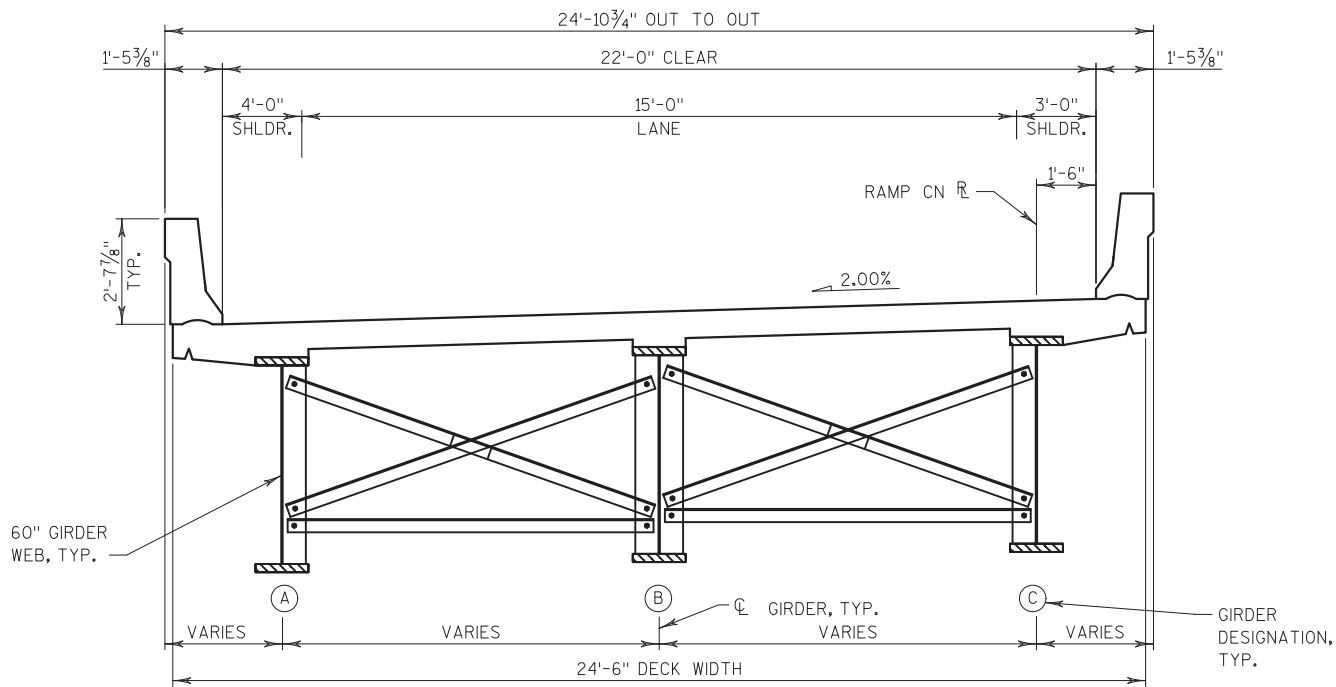
- DRAWINGS SHALL NOT BE SCALED.
- DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS.
- AREAS TO BE CLEANED AND PAINTED INCLUDE ALL STRUCTURAL METAL SURFACES, INCLUDING GALVANIZED ITEMS NOTED IN THE PLAN DETAILS. 1,678,600 SF AREA IS APPROXIMATE AND IS GIVEN FOR INFORMATIONAL PURPOSES ONLY.
- THE FINISH PAINT COLOR SHALL BE FEDERAL COLOR NO. 15450.
- "REPLACE BOLTS" TO INCLUDE  $\frac{3}{4}$ " AND  $\frac{7}{8}$ " BOLTS. BOLTS TO BE REPLACED AS DETERMINED BY THE ENGINEER.

TOTAL ESTIMATED QUANTITIES

| ITEM NO.    | BID ITEM   | TOTAL   |
|-------------|--|---------|
| SPV.0060.01 | REPLACE BOLTS  | 60 EACH |
| SPV.0105.01 | STRUCTURE REPAINTING RECYCLED ABRASIVE PROJECT 1300-13-72                          | 1 LS    |
| SPV.0105.02 | PAINTING WARRANTED PROJECT 1300-13-72  | 1 LS    |
| SPV.0105.03 | NEGATIVE PRESSURE CONTAINMENT AND COLLECTION OF WASTE MATERIALS PROJECT 1300-13-72 | 1 LS    |
| SPV.0105.04 | PORTABLE DECONTAMINATION FACILITIES PROJECT 1300-13-72                             | 1 LS    |

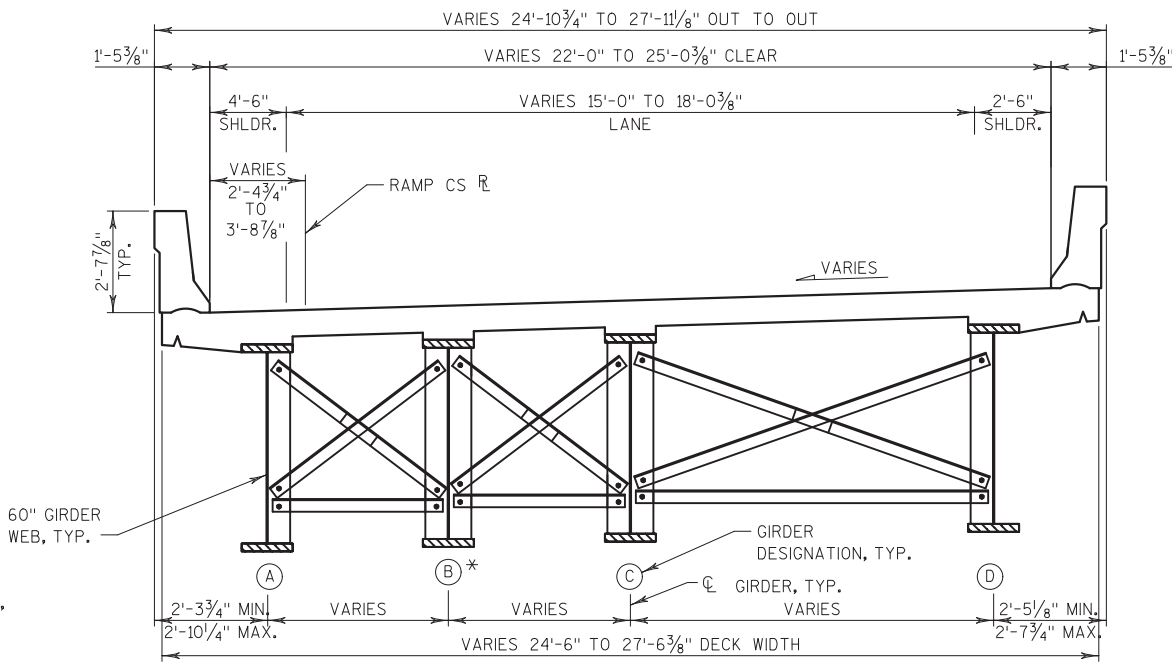
|  |      |                    |                |
|--|------|--------------------|----------------|
|  |      |                    |                |
| NO.  | DATE | REVISION           | BY             |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |                |
| STRUCTURE B-40-400                                 |      |                    |                |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |                |
| ELEVATIONS,<br>GENERAL NOTES<br>AND QUANTITIES     |      |                    | SHEET 10 OF 15 |

FILE=  
SCALE =



TYPICAL CROSS SECTION - UNIT S24

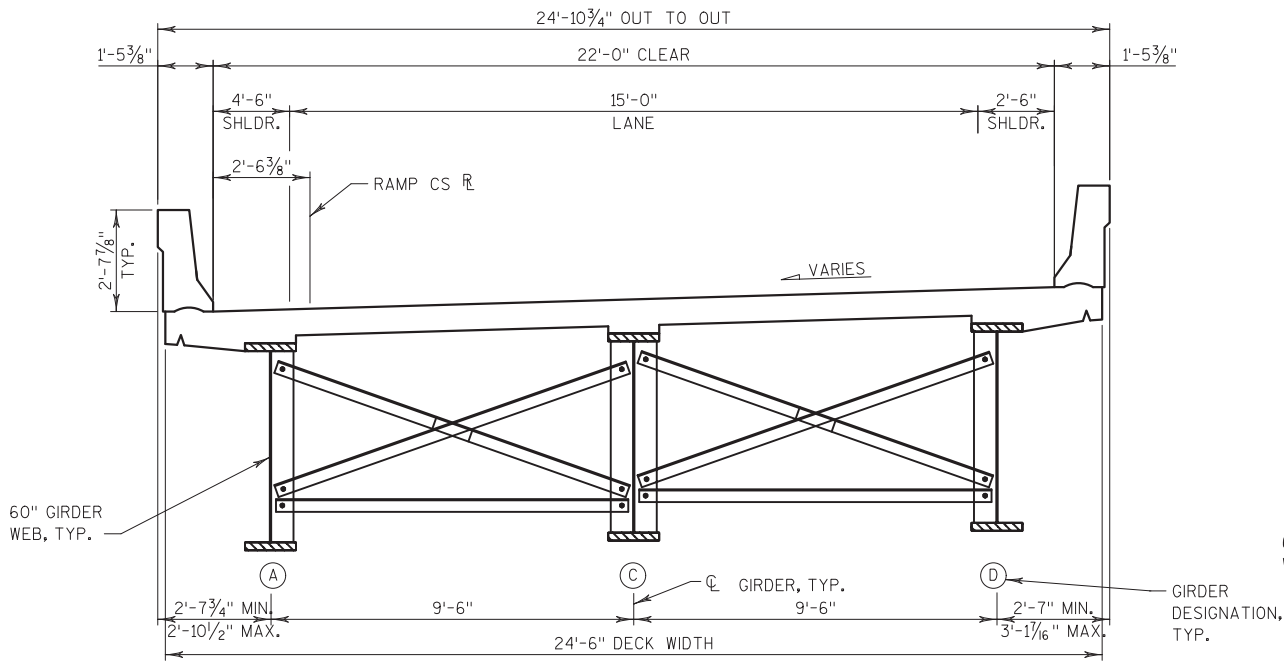
(LOOKING NORTH)



TYPICAL CROSS SECTION - UNIT S23

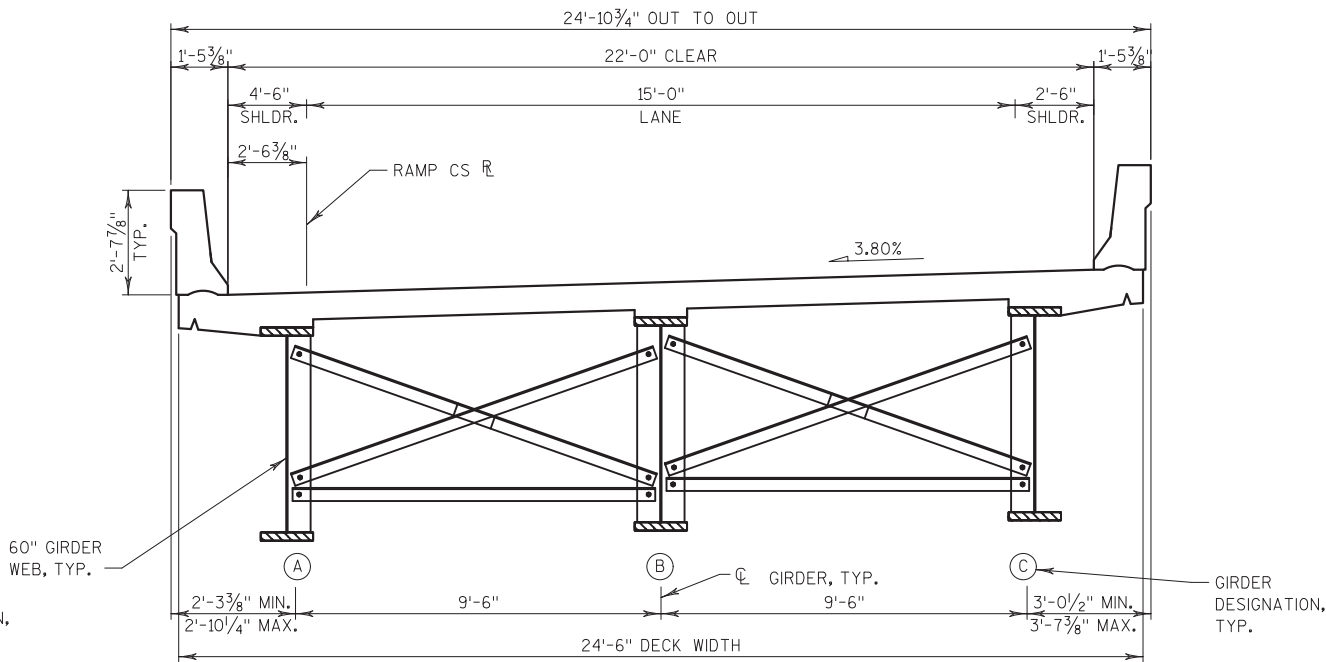
SOUTH OF STATION 362+87.87 CS (LOOKING NORTH)

\* SOUTH END OF SPAN 1 ONLY



TYPICAL CROSS SECTION - UNIT S23

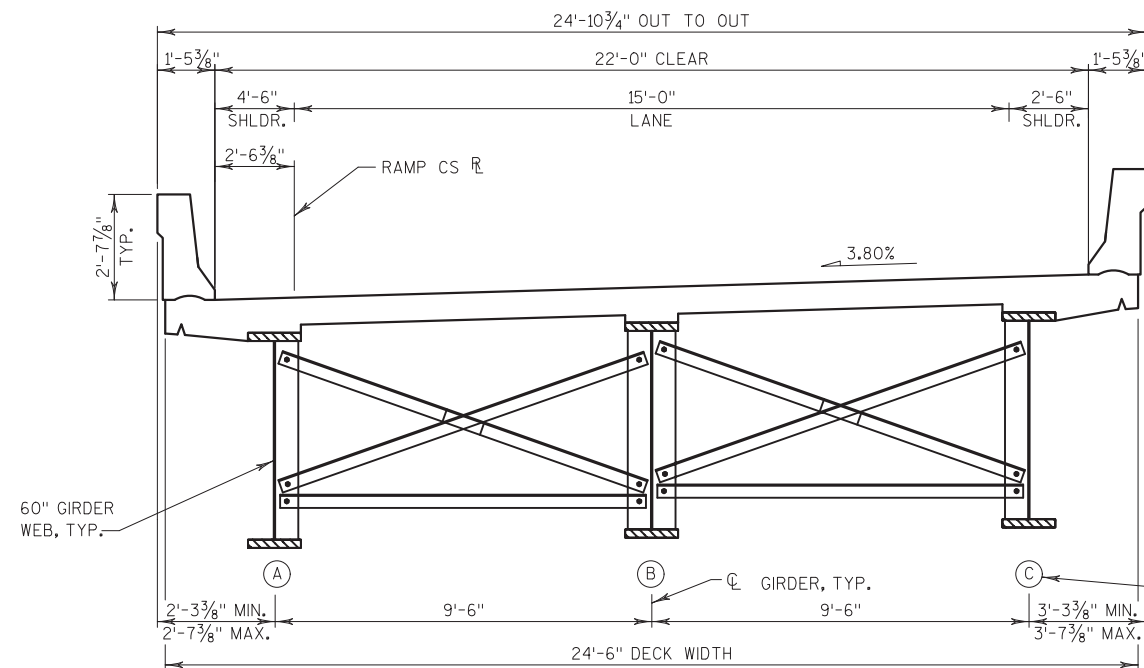
NORTH OF STATION 362+87.87 CS (LOOKING NORTH)



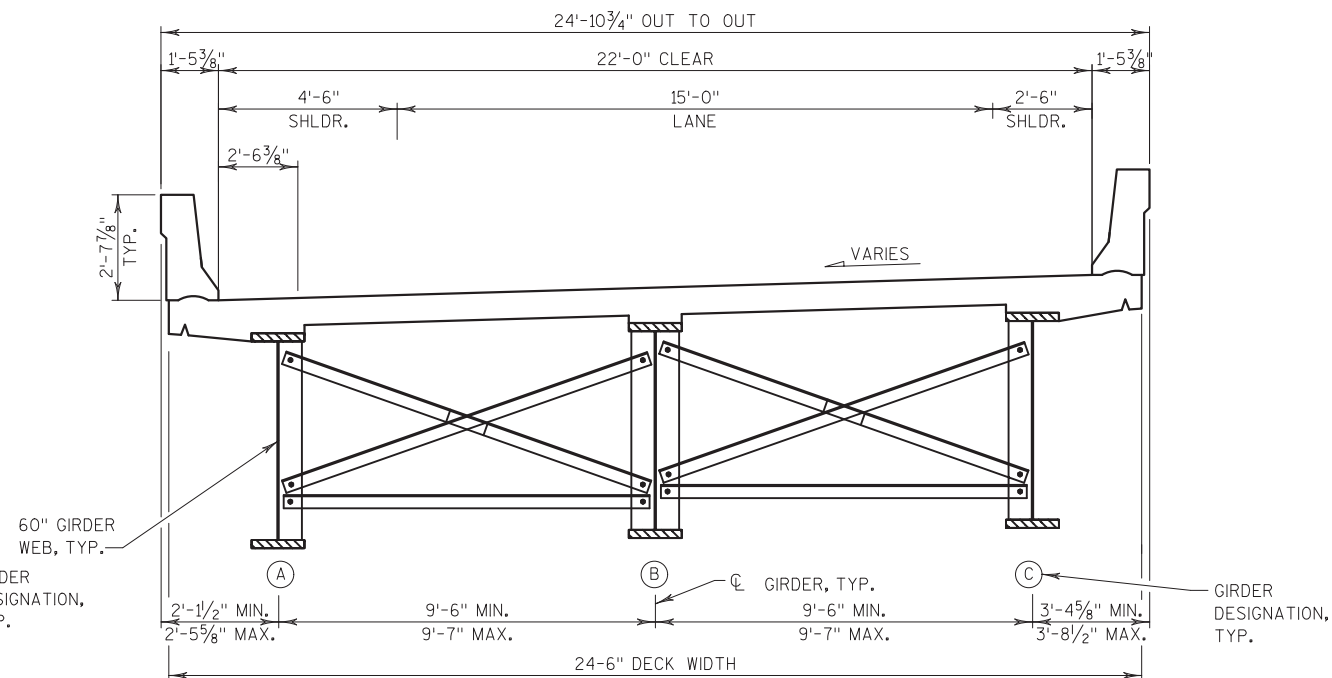
TYPICAL CROSS SECTION - UNIT S22

(LOOKING NORTH)

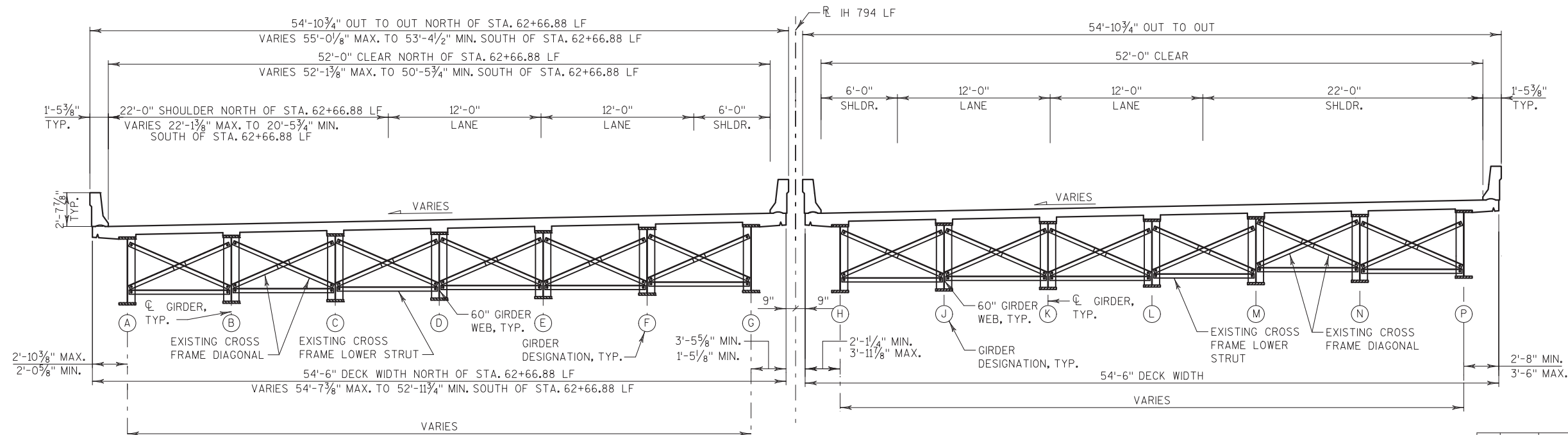
| NO.  | DATE | REVISION           | BY |
|--|------|--------------------|----|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |    |
| STRUCTURE B-40-400                                 |      |                    |    |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |    |
| TYPICAL SECTIONS<br>1                              |      | SHEET 11 OF 15     |    |



**TYPICAL CROSS SECTION - UNIT S21**  
(LOOKING NORTH)



**TYPICAL CROSS SECTION - UNIT S20**  
(LOOKING NORTH)

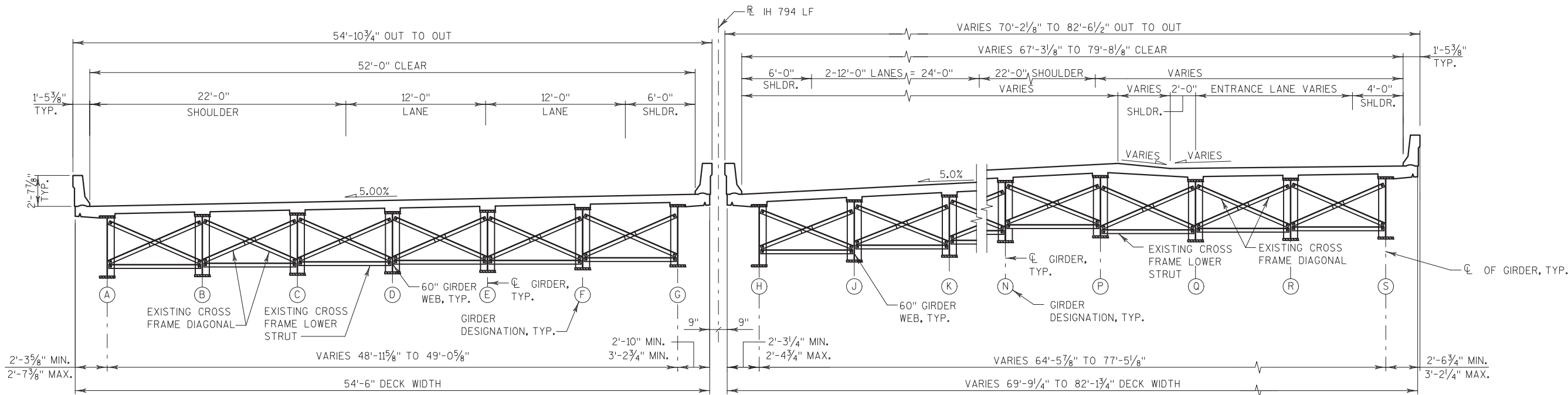


**UNIT S19-B**

**TYPICAL CROSS SECTION**  
UNIT S19  
(LOOKING NORTH)

**UNIT S19-A**

| NO.  | DATE | REVISION        | BY |
|--|------|-----------------|----|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                 |    |
| STRUCTURE B-40-400                                 |      |                 |    |
| DRAWN BY MSC                                       |      | PLANS CK'D. CDH |    |
| TYPICAL SECTIONS<br>2                              |      | SHEET 12 OF 15  |    |

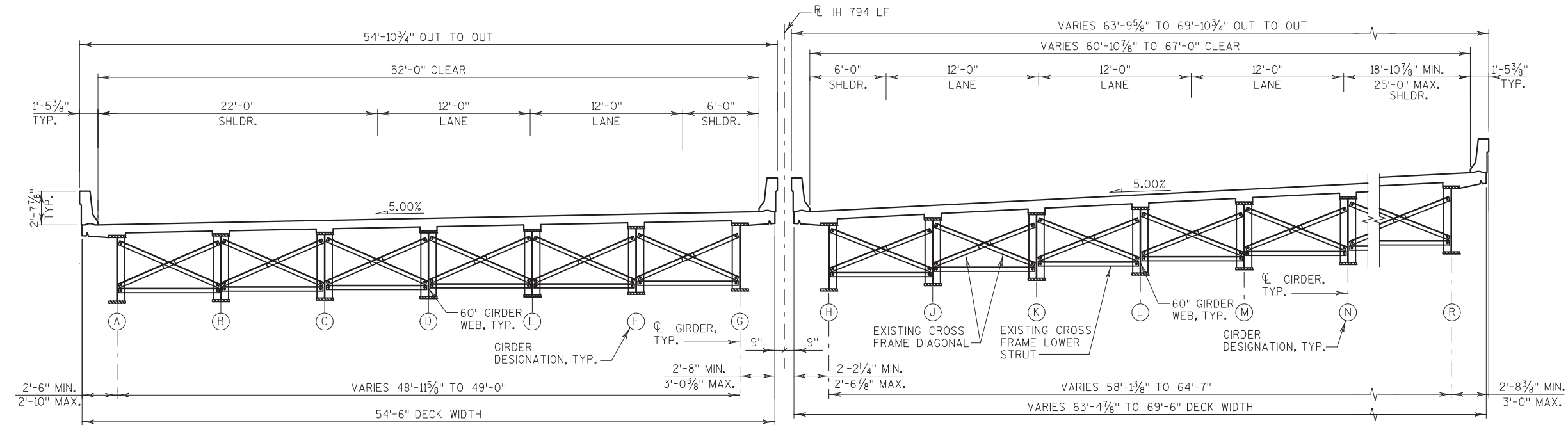


UNIT S18-B

TYPICAL CROSS SECTION

UNIT S18  
(LOOKING NORTH)

UNIT S18-A



UNIT S17-B

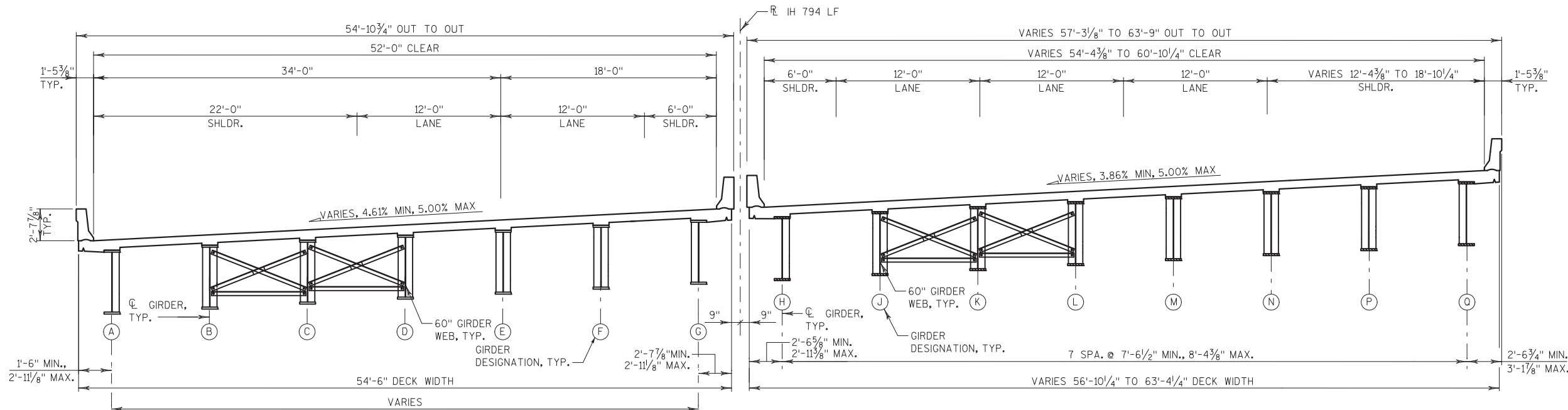
TYPICAL CROSS SECTION

UNIT S17  
(LOOKING NORTH)

UNIT S17-A

| NO.  | DATE | REVISION           | BY |
|--|------|--------------------|----|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |    |
| STRUCTURE B-40-400                                 |      |                    |    |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |    |
| TYPICAL SECTIONS<br>3                              |      | SHEET 13 OF 15     |    |



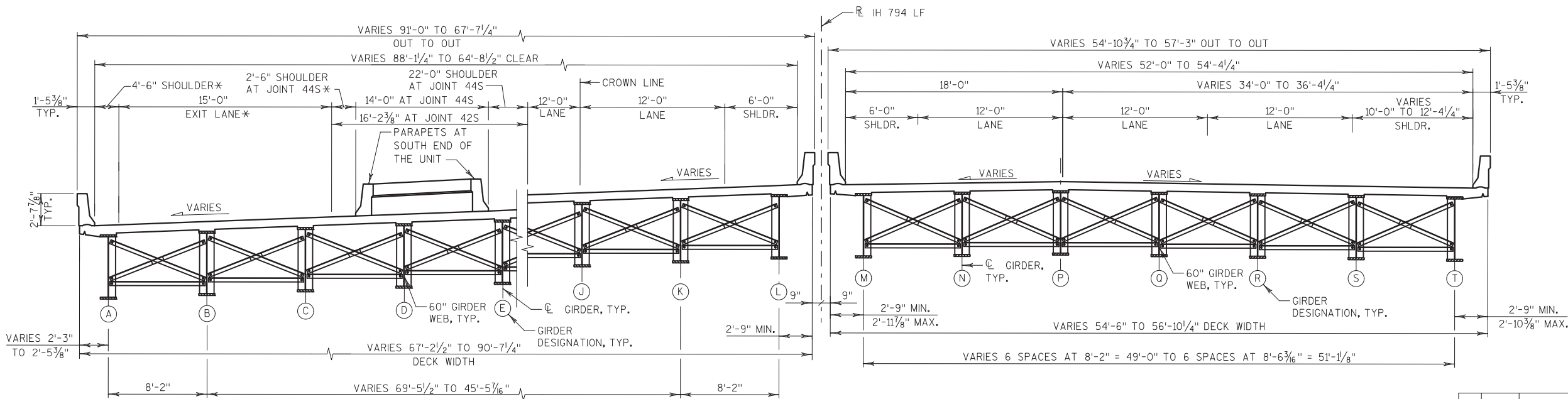


UNIT S16-B

TYPICAL CROSS SECTION

UNIT S16  
(LOOKING NORTH)

UNIT S16-A



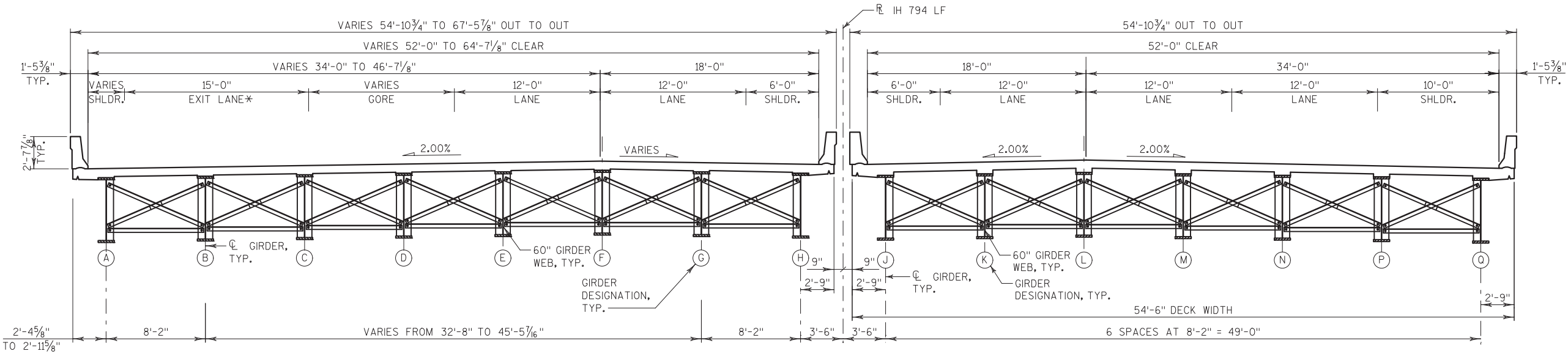
UNIT S15-B

TYPICAL CROSS SECTION

UNIT S15  
(LOOKING NORTH)  
\* DIMENSION NORMAL TO EXIT LANE

UNIT S15-A

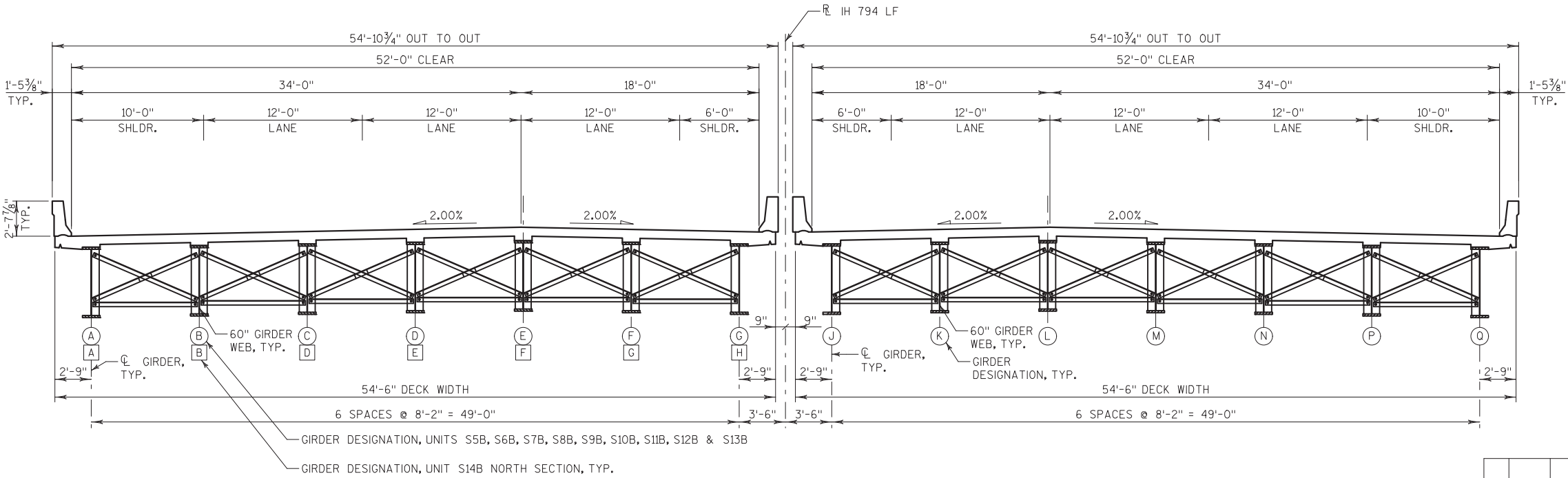
| NO.  | DATE | REVISION           | BY |
|--|------|--------------------|----|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                    |    |
| STRUCTURE B-40-400                                 |      |                    |    |
| DRAWN<br>BY MSC                                    |      | PLANS<br>CK'D. CDH |    |
| TYPICAL SECTIONS<br>4                              |      | SHEET 14 OF 15     |    |



UNIT S14-B  
(SOUTH SECTION)

TYPICAL CROSS SECTION  
UNIT S14  
(LOOKING NORTH)

UNIT S14-A  
(SOUTH SECTION & NORTH SECTION)



UNITS S5B, S6B, S7B, S8-B, S9-B, S10-B,  
S11-B, S12-B, S13-B & S14-B (NORTH SECTION)

TYPICAL CROSS SECTION  
UNITS S5, S6, S7, S8, S9, S10, S11, S12, S13 & S14  
(LOOKING NORTH)

UNITS S5A, S6A, S7A, S8-A, S9-A, S10-A,  
S11-A, S12-A, S13-A, & S14-A

|  |      |                 |                    |
|--|------|-----------------|--------------------|
|  |      |                 |                    |
| NO.  | DATE | REVISION        | BY                 |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION |      |                 |                    |
| STRUCTURE B-40-400                                 |      |                 |                    |
|  |      | DRAWN<br>BY MSC | PLANS<br>CK'D. CDH |
| TYPICAL SECTIONS<br>5                              |      | SHEET 15 OF 15  |                    |
|  |      |                 |                    |

## Notes



## ***Wisconsin Department of Transportation***

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