GRE

AUG 2015

PROJECT ID: 4100-31-71

COUNTY: CALUMET

ORDER OF SHEETS

TOTAL SHEETS = 138

07

N

DESIGN DESIGNATION

A.A.D.T.	2016	= 9,000
A.A.D.T.	2036	= 11,200
D.H.V.		= 7.6
D.D.		= 50/50
T.		= 8.7%
DESIGN SPEED		= 35 MPH
ESALS		= 3,496,700

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	ROCK
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	LABEL
EXISTING RIGHT OF WAY		GRADE ELEVATION	95.36
PROPOSED OR NEW R/W LINE			
SLOPE INTERCEPT		CULVERT (Profile View)	
REFERENCE LINE		UTILITIES	
EXISTING CULVERT		ELECTRIC	E
PROPOSED CULVERT (Box or Pipe)		FIBER OPTIC	FO
		GAS	G
COMBUSTIBLE FLUIDS	CAUTION	SANITARY SEWER	SAN
		STORM SEWER	SS
		TELEPHONE	T
MARSH AREA		WATER	W
		UTILITY PEDESTAL	
WOODED OR SHRUB AREA		POWER POLE	
		TELEPHONE POLE	

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
4100-31-71	WISC 2015469	1

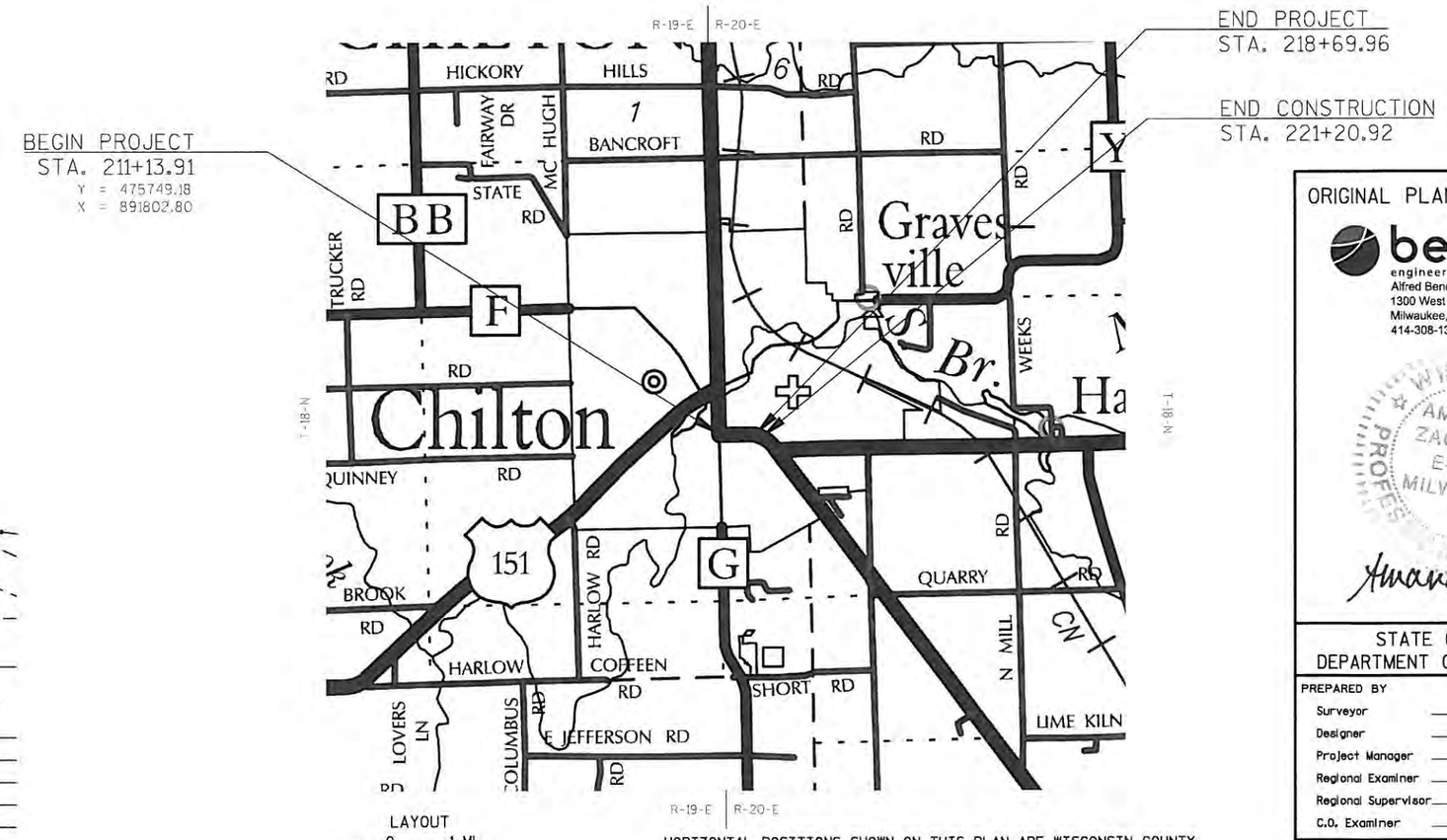
E. CHESTNUT STREET, CITY OF CHILTON

S. MADISON STREET - ELM STREET

USH 151

CALUMET COUNTY

STATE PROJECT NUMBER
4100-31-71



LAYOUT
SCALE 0 1 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.143

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, CALUMET COUNTY, NAD83 (1991), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ORIGINAL PLANS PREPARED BY

benesch
engineers · scientists · planners
Alfred Benesch & Company
1300 West Canal Street, Suite 150
Milwaukee, Wisconsin 53233
414-308-1310 Job No. 20140.00

WISCONSIN AMANDA E. ZACHARIAS E-35716 MILWAUKEE WI PROFESSIONAL ENGINEER

Amanda Zacharias

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WISDOT
Designer	ALFRED BENESCH & CO
Project Manager	TIM VERHAGEN
Regional Examiner	
Regional Supervisor	CHUCK KAROW
C.O. Examiner	

APPROVED FOR THE DEPARTMENT

DATE: 4/27/2015 Tim Verhagen (Signature)

E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\010101_TI.DWG

PLOT DATE : 4/20/2015 2:12 PM

PLOT BY : WATT, TIMOTHY

PLOT NAME :

WISDOT/CADDS SHEET 10

GENERAL NOTES

1. NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.
2. UNLESS NOTED ON THE PLAN, ALL STATIONS AND OFFSETS ARE REFERENCED FROM ℄ USH 151.
3. DISTURBED AREAS WITHIN THE RIGHT-OF-WAY SHALL BE FINISHED WITH SOD.
4. EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
5. EXISTING DRIVEWAYS BEHIND THE BACK OF WALK SHALL BE RESTORED IN KIND AS DIRECTED BY THE ENGINEER IN THE FIELD AT THE LOCATIONS DETERMINED BY THE ENGINEER.
6. TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. NO WORK MAY BEGIN UNTIL PROPER TRAFFIC CONTROL DEVICES ARE PLACED AND APPROVED BY THE ENGINEER.
7. A SAWED JOINT IS REQUIRED WHERE NEW PAVEMENT MEETS EXISTING PAVEMENT.
8. ALL ELEVATIONS AND OFFSETS SHOWN IN THE PLAN SHALL BE VERIFIED IN THE FIELD.
9. THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
10. THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPALITY OR PUBLIC AGENCY WHICH IS NOT A MEMBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.
11. EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION AND SHALL BE MAINTAINED UNTIL SUCH TIME AS THE ENGINEER DEEMS THE MEASURE NO LONGER NECESSARY.
12. CROSS SECTIONS ARE REFERENCED TO THE ℄ USH 151.
13. THE LOCATIONS OF EXISTING AND PROPOSED UTILITY FACILITIES (WITH THE EXCEPTION OF SANITARY SEWER AND WATERMAIN) AS SHOWN ON THE PLAN AND CROSS SECTIONS ARE APPROXIMATE AND ARE BASED ON UTILITY LOCATES OBTAINED FROM THE UTILITIES WHEN THE SURVEY DATA WAS COLLETED PRIOR TO THE FINAL DESIGN OF THE PROJECT. IT IS ANTICIPATED THAT UTILITY FACILITIES WILL BE RELOCATED AS PART OF THIS PROJECT AS DESCRIBED IN THE SPECIAL PROVISIONS. LOCATIONS OF THE PROPOSED RELOCATED UTILITY FACILITIES ARE NOT SHOWN ON THE PLAN SHEETS OR CROSS SECTIONS.

UTILITY CONTACTS

CHARTER COMMUNICATIONS (COMMUNICATION LINE)
3315 LINCOLN AVE
TWO RIVERS, WI 54241
MR. NICK FRASE
(920) 793-2216
nicholas.frase@chartercom.com

CHILTON LAKE DISTRICT - WATER
42 SCHOOL STREET
CHILTON, WI 53014
MR. TODD SCHWARZ
(920) 849-2451 EXT. 320
chiltondpw@chiltonwi.com

FRONTIER NORTH, INC (COMMUNICATION LINE)
100 COMMUNICATIONS DRIVE
SUN PRAIRIE, WI 53590
MR. RYAN OSNESS
(608) 837-1881
ryan.d.osness@ftr.com

WISCONSIN PUBLIC SERVICE CORPORATION (GAS/PETROLEUM)
933 S WILDWOOD AVE
SHEBOYGAN, WI 53081
MR. MIKE LOWTHER
(920) 451-3743
(920) 946-3198 (MOBILE)
mllowther@wisconsinpublicservice.com

WISCONSIN PUBLIC SERVICE CORPORATION (ELECTRICITY)
P.O. BOX 236
TWO RIVERS, WI 54241
MR. JEFF PELISCHEK
(920) 657-1816
(920) 323-4836 (MOBILE)
jpelischek@wisconsinpublicservice.com

CHILTON LAKE DISTRICT - SANITARY SEWER
42 SCHOOL STREET
CHILTON, WI 53014
MR. TODD SCHWARZ
(920) 849-2451 EXT. 320
chiltondpw@chiltonwi.com

OTHER AGENCIES

DNR
DNR NORTHEAST REGION
2984 SHAWANO AVE
GREEN BAY, WI 54313
MR. MATTHEW SCHAEVE
(920) 662-5472
matthew.schaeve@wisconsin.gov

CITY OF CHILTON
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CHILTON, WI 53014
MR. TODD SCHWARZ
(920) 849-2451 EXT. 320
chiltondpw@chiltonwi.com

DESIGN
ALFRED BENESCH & COMPANY
1300 W CANAL ST, SUITE 150
MILWAUKEE, WI 53233
MS. AMANDA ZACHARIAS
(414) 308-1320
azacharias@benesch.com

WISDOT
WISCONSIN DOT, NORTHEAST REGION
944 VANDERPERRE WAY
GREEN BAY, WI 54324
MR. TIM VERHAGEN
(920) 492-4116
timothy.verhagen@dot.wi.gov

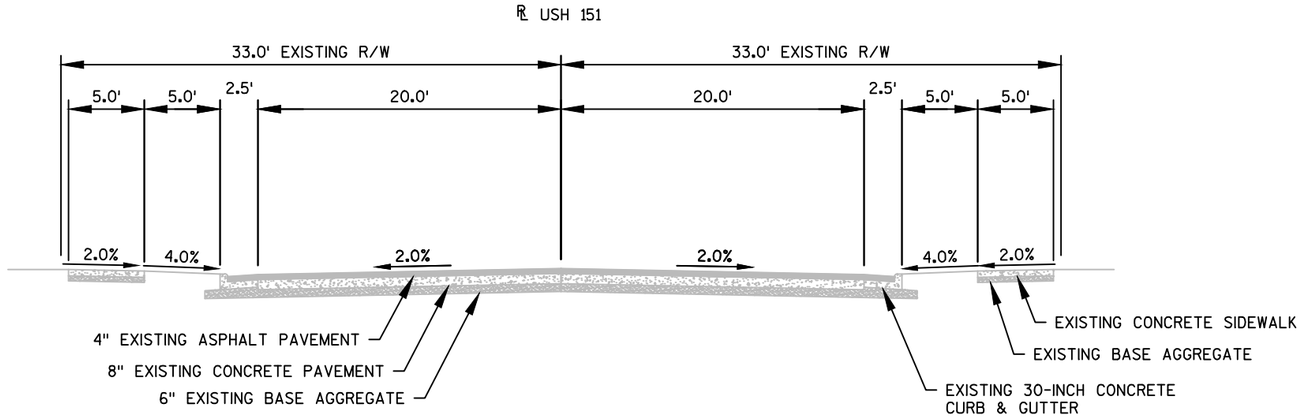
LIST OF STANDARD ABBREVIATIONS

ABUT	ABUTMENT
AC	ACRE
AGG	AGGREGATE
AADT	ANNUAL AVERAGE DAILY TRAFFIC
ASPH	ASPHALT
AVG	AVERAGE
ADT	AVERAGE DAILY TRAFFIC
BF	BACK FACE
BL	BASE LINE
BM	BENCH MARK
CB	CATCH BASIN
C/L	CENTERLINE
CC	CENTER TO CENTER
CE	COMMERCIAL ENTRANCE
CONC	CONCRETE
CO	COUNTY
CTH	COUNTY TRUNK HIGHWAY
CY	CUBIC YARD
CULV	CULVERT
C&G	CURB AND GUTTER
DHV	DESIGN HOUR VOLUME
DIA	DIAMETER
DD	DIRECTIONAL DISTRIBUTION
E	EAST
X	EAST GRID COORDINATE
EB	EASTBOUND
ELEC	ELECTRIC
ELEV	ELEVATION
ESALS	EQUIVALENT SINGLE AXLE LOADS
EXC	EXCAVATION
EBS	EXCAVATION BELOW SUBGRADE
EXIST	EXISTING
FF	FACE TO FACE
FE	FIELD ENTRANCE
FG	FINISHED GRADE
FL	FLOW LINE
FT	FOOT
FTMS	FREEWAY TRAFFIC MANAGEMENT SYSTEM
HES	HIGH EARLY STRENGTH
CWT	HUNDREDWEIGHT
HYD	HYDRANT
IN DIA	INCH DIAMETER
INL	INLET
ID	INSIDE DIAMETER
INV	INVERT
IP	IRON PIPE
JT	JOINT
JCT	JUNCTION
LT	LEFT
L	LENGTH OF CURVE
LHF	LEFT-HAND FORWARD
LF	LINEAR FOOT
L	LITER
LS	LUMP SUM
MH	MANHOLE
MB	MESSAGE BOARD
MLB	MAILBOX
ML	MATCH LINE
NC	NORMAL CROWN
N	NORTH
Y	NORTH GRID COORDINATE
NB	NORTHBOUND
NO	NUMBER
OD	OUTSIDE DIAMETER
PAVT	PAVEMENT
PERM	PERMANENT
PLE	PERMANENT LIMITED EASEMENT
PT	POINT
PVC	POLYVINYL CHLORIDE
PCC	PORTLAND CEMENT CONCRETE
PE	PRIVATE ENTRANCE
PROJ	PROJECT
PL	PROPERTY LINE
R	RADIUS
R/L	REFERENCE LINE
RCCP	REINFORCED CONCRETE CULVERT PIPE
REQD	REQUIRED
RT	RIGHT
RHF	RIGHT HAND FORWARD
R/W	RIGHT-OF-WAY
RD	ROAD
RDWY	ROADWAY
SALV	SALVAGED
SSS	SANITARY AND STORM SEWER
SAN S	SANITARY SEWER
SEC	SECTION
SHLDR	SHOULDER
SW	SIDEWALK
S	SOUTH
SB	SOUTHBOUND
SP	SPECIAL
SPECS	SPECIFICATIONS
SQ	SQUARE
SF	SQUARE FEET
SY	SQUARE YARD
STD	STANDARD
SDD	STANDARD DETAIL DRAWINGS
STH	STATE TRUNK HIGHWAYS
STA	STATION
SS	STORM SEWER
STR	STRUCTURE OR STRUCTURAL
SL	SURVEY LINE
TEL	TELEPHONE
TEMP	TEMPORARY
TLE	TEMPORARY LIMITED EASEMENT
T	TON
TC	TOP OF CURB
T	TRUCKS (PERCENT OF)
TYP	TYPICAL
UG	UNDERGROUND
USH	UNITED STATES HIGHWAY
VAR	VARIABLE
VERT	VERTICAL
W	WATER
WM	WATER MAIN
WV	WATER VALVE
W	WEST
WB	WESTBOUND
YD	YARD

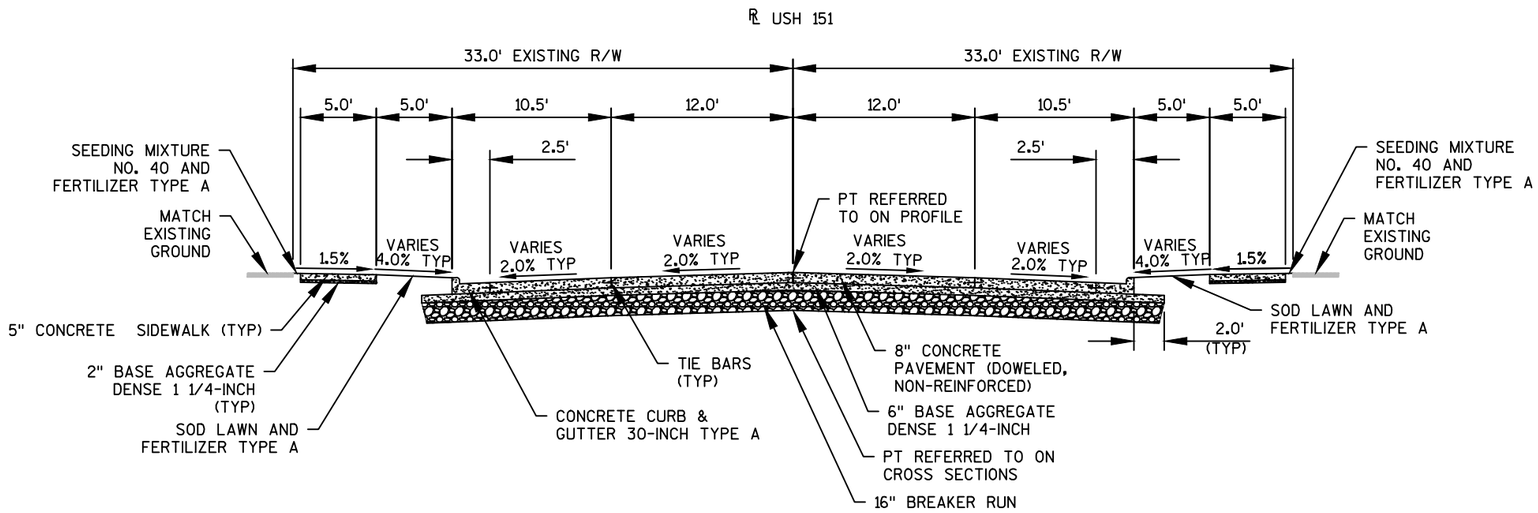
ORDER OF SECTION 2 SHEETS

2

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PROJECT OVERVIEW	SHEET 3	E



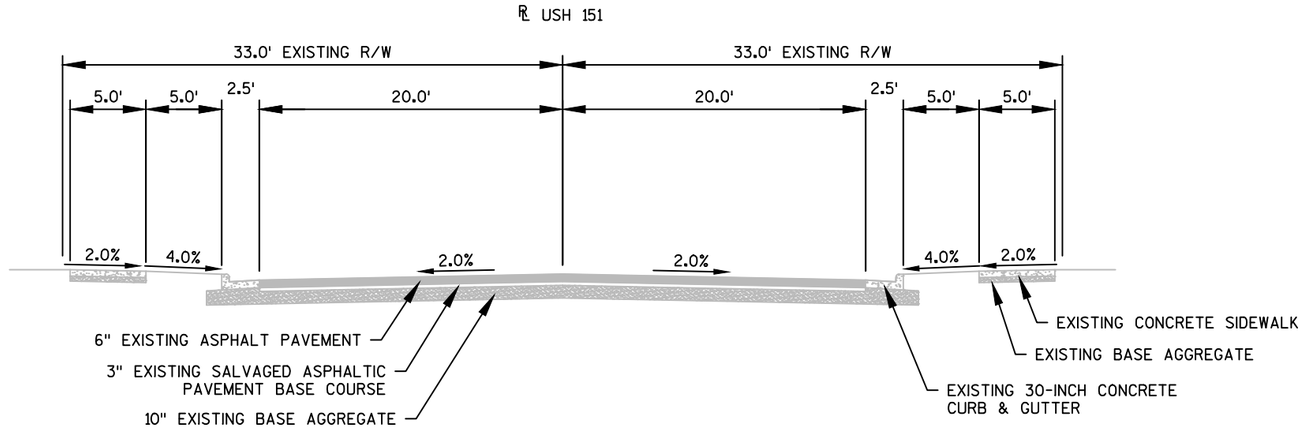
TYPICAL EXISTING SECTION
USH 151
STA 211+13.91 TO STA 218+69.96



TYPICAL FINSHED SECTION
USH 151
STA 211+13.91 TO STA 218+69.96

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	TYPICAL SECTIONS	SHEET 4	E

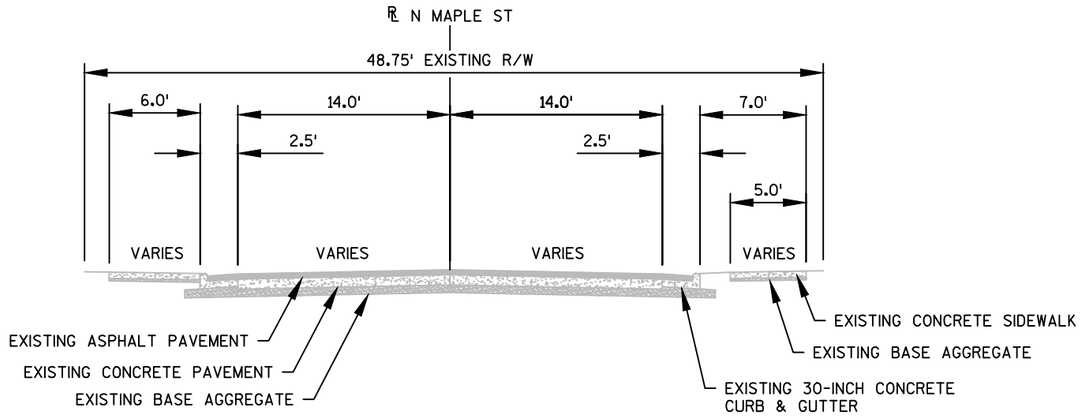
2



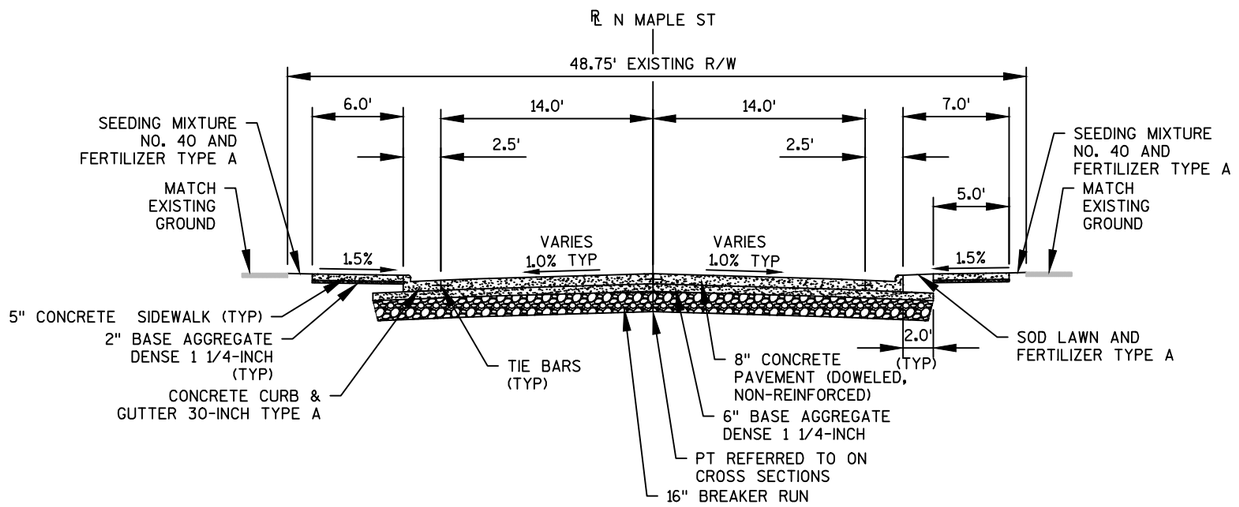
TYPICAL EXISTING SECTION
USH 151
STA 218+69.96 TO STA 221+20.92

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	TYPICAL SECTIONS	SHEET 5	E

2

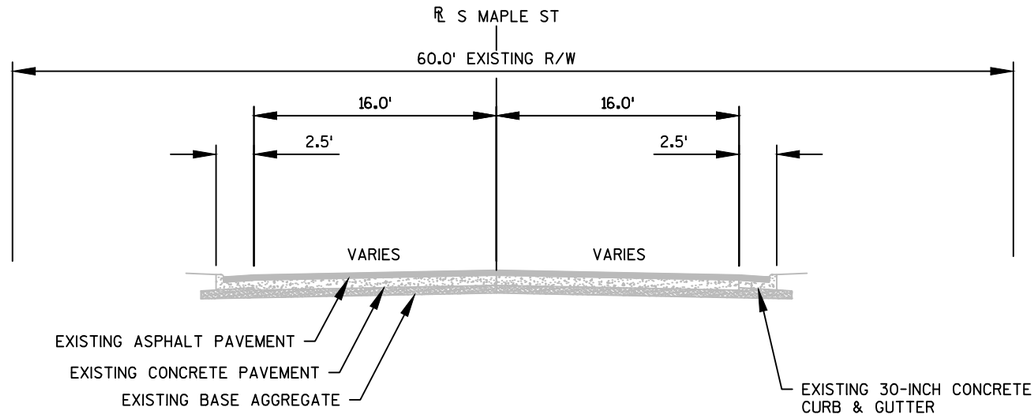


TYPICAL EXISTING SECTION
N MAPLE ST
STA 10+24.01 TO STA 10+49.28

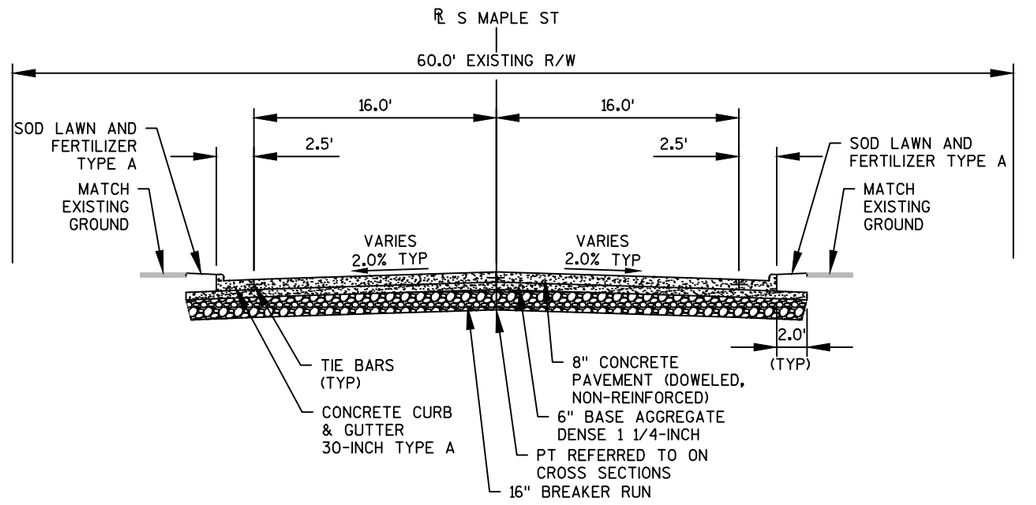


TYPICAL FINSHED SECTION
N MAPLE ST
STA 10+24.01 TO STA 10+49.28

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	TYPICAL SECTIONS	SHEET 6	E



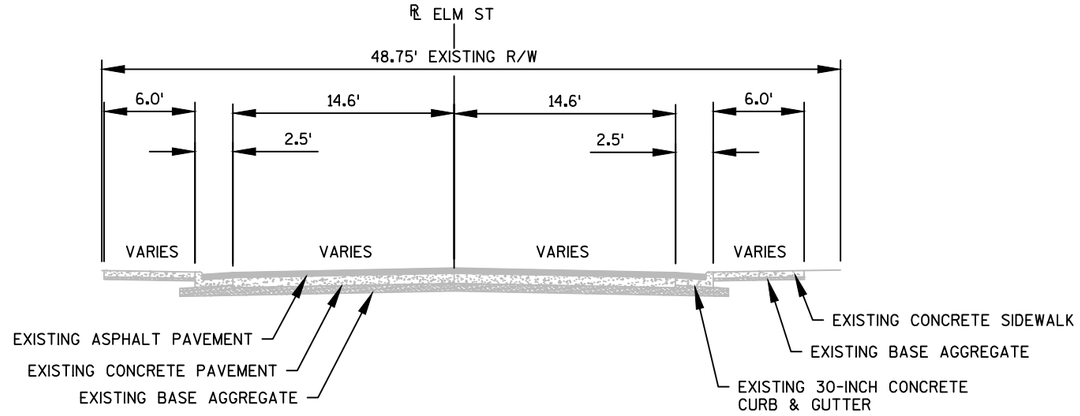
TYPICAL EXISTING SECTION
S MAPLE ST
STA 15+20.00 TO STA 15+47.97



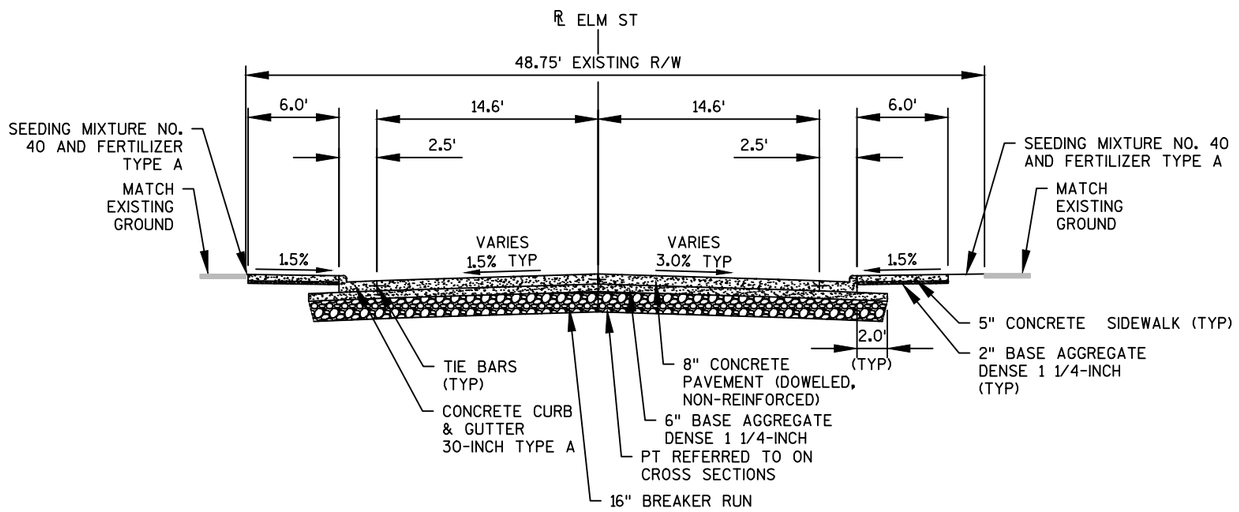
TYPICAL FINSHED SECTION
S MAPLE ST
STA 15+20.00 TO STA 15+47.97

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	TYPICAL SECTIONS	SHEET 7	E

2



TYPICAL EXISTING SECTION
ELM ST
STA 20+25.36 TO STA 20+53.54

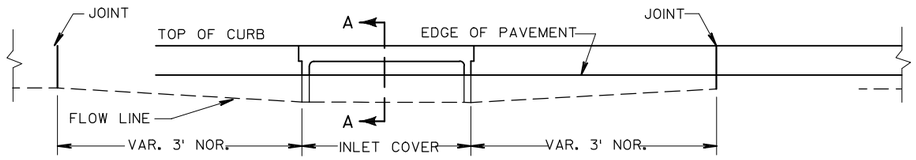


TYPICAL FINSHED SECTION
ELM ST
STA 20+25.36 TO STA 20+53.54

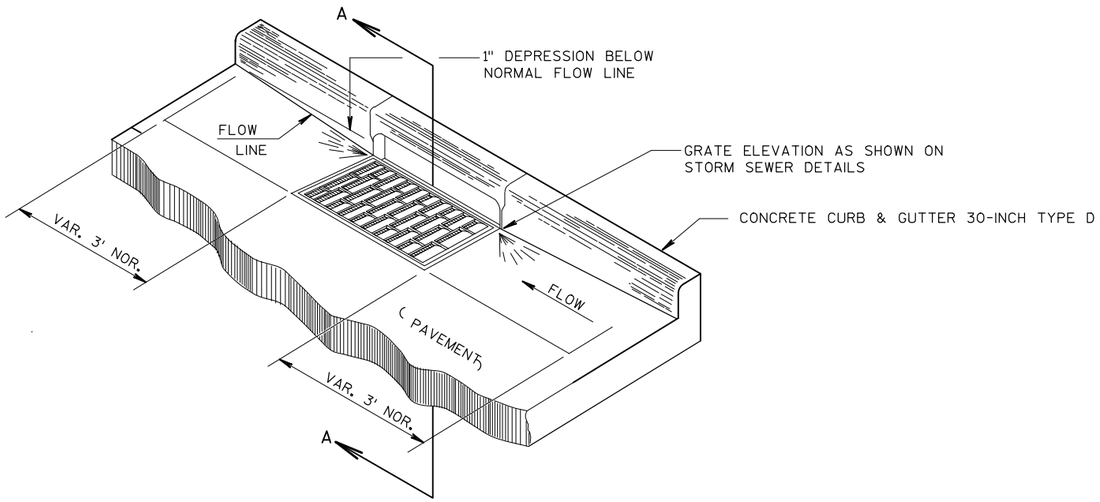
PROJECT NO: XXXX-XX-XX	HWY: XXXX	COUNTY: XXXX	PLAN: TYPICAL SECTION	SHEET 8	E

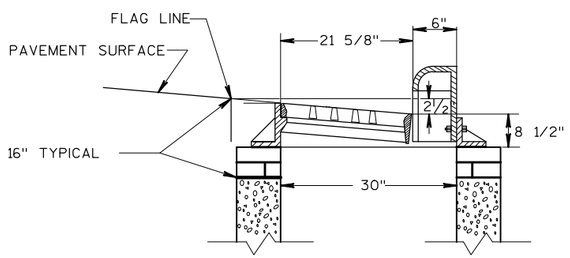
FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\020301_TS.DWG PLOT DATE : 4/22/2015 9:59 AM PLOT BY : WATT, TIMOTHY PLOT NAME : ---------- PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

2



ELEVATION

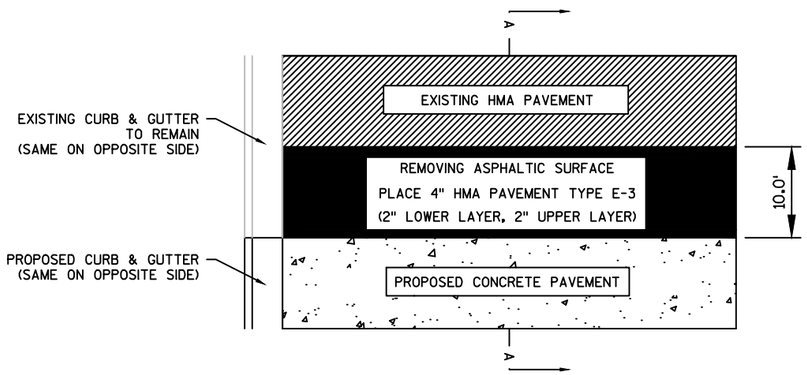




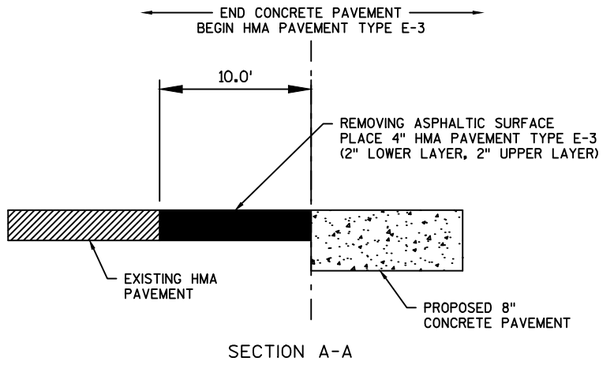
SECTION A-A

DETAIL OF CURB AND GUTTER AT INLETS

(TYPE WM INLET SHOWN)



PLAN VIEW



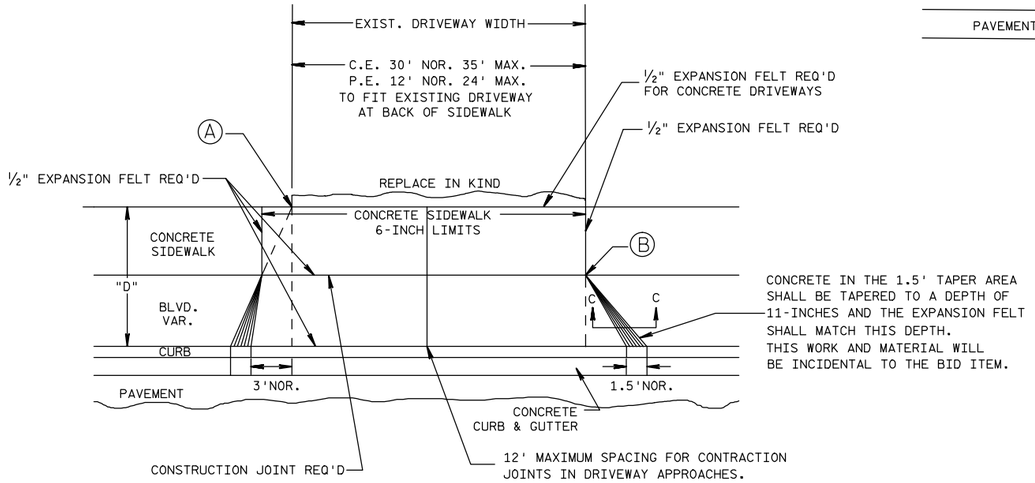
SECTION A-A

SIDE ROAD ASPHALT TRANSITION

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CONSTRUCTION DETAILS	SHEET 9	E

2

DRIVEWAY ENTRANCE DETAIL WITH SIDEWALK, CURB & GUTTER

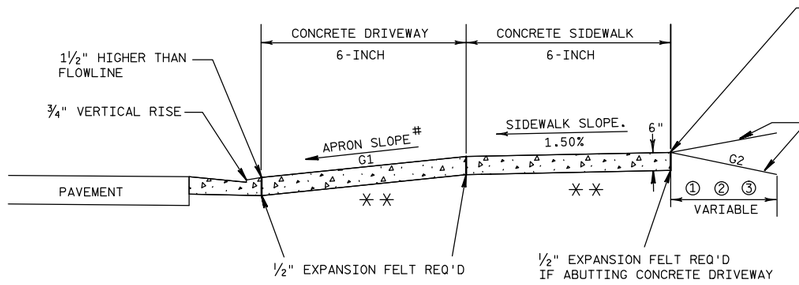


CONCRETE IN THE 1.5' TAPER AREA SHALL BE TAPERED TO A DEPTH OF 11-INCHES AND THE EXPANSION FELT SHALL MATCH THIS DEPTH. THIS WORK AND MATERIAL WILL BE INCIDENTAL TO THE BID ITEM.

PLAN VIEW

Ⓐ WHEN "D" IS 13' OR LESS, ALIGN TAPER WITH BACK OF SIDEWALK

Ⓑ WHEN "D" IS GREATER THAN 13', ALIGN TAPER WITH FRONT OF SIDEWALK



TYPICAL SIDEWALK SECTION

①—6" CONCRETE DRIVEWAY OR H.E.S. CONCRETE DRIVEWAY

②— 6" BASE AGG. DENSE 1 1/4-INCH

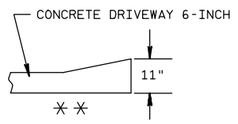
③— 4" BASE AGG. DENSE 1 1/4-INCH BASE WITH 2" ASPHALTIC SURFACE

** = 6" BASE AGG. DENSE 1 1/4-INCH REQ'D UNDER CONCRETE DRWY

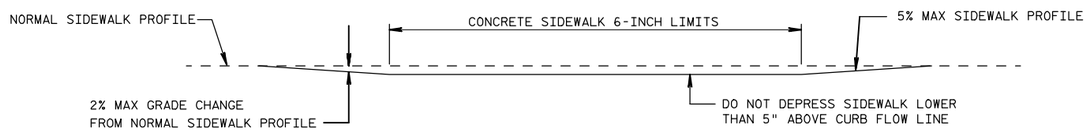
#

TERRACE WIDTH	APRON SLOPE (G1) MIN %	DESIRABLE %	MAX %
5 FT	4.0	7.0	9.0

NOTE: ALGEBRAIC DIFFERENCE BETWEEN TANGENT GRADES G1 & G2 TO NOT EXCEED 14%

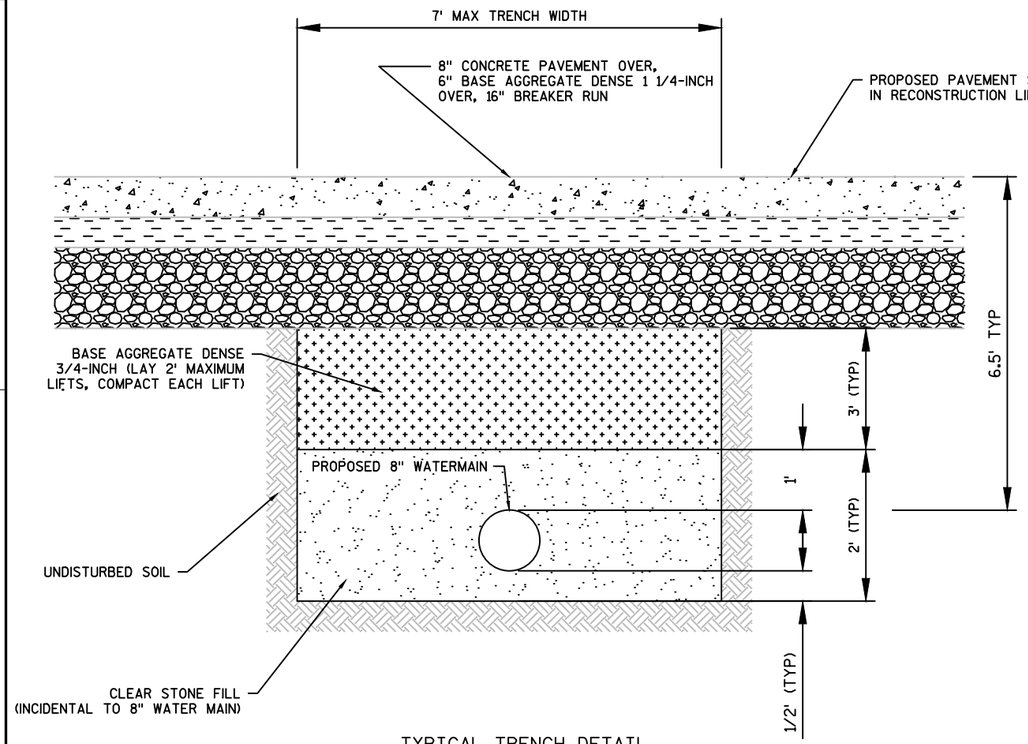


SECTION C-C



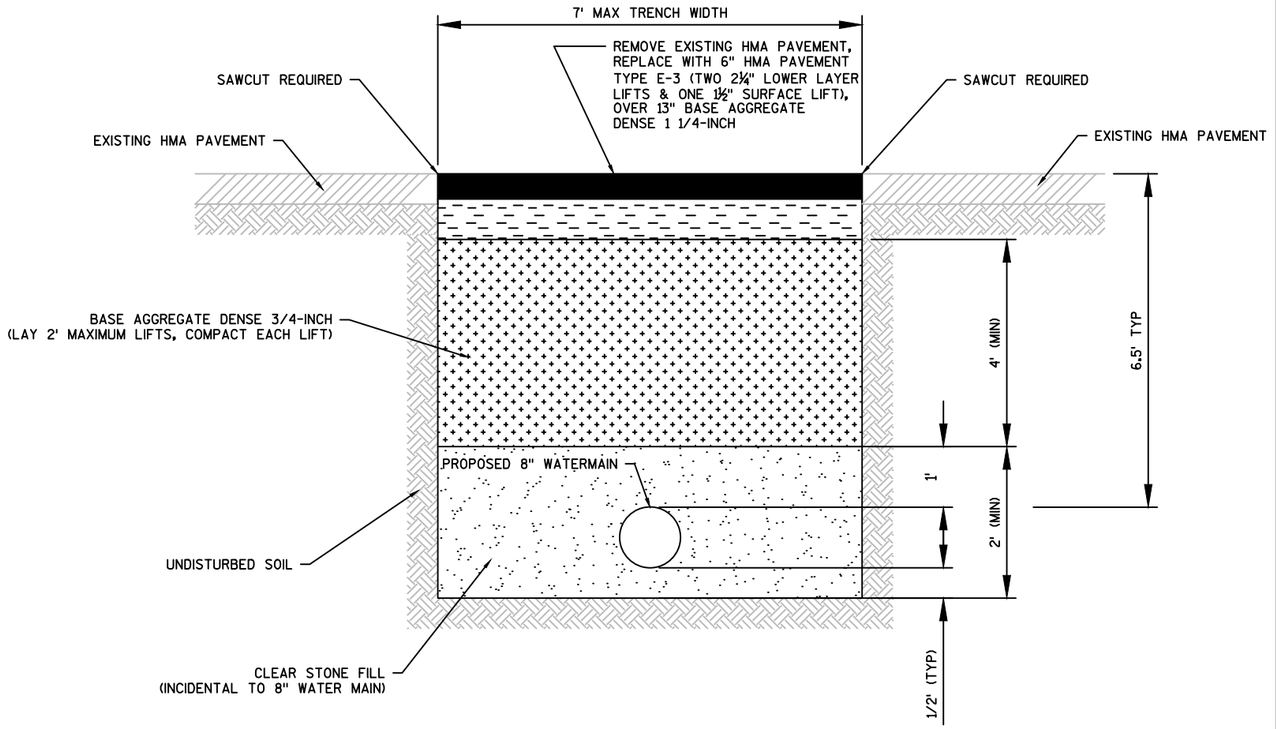
DEPRESSED SIDEWALK PROFILE DETAIL

2



TYPICAL TRENCH DETAIL

STA. 211+13.91 TO STA. 218+69.96

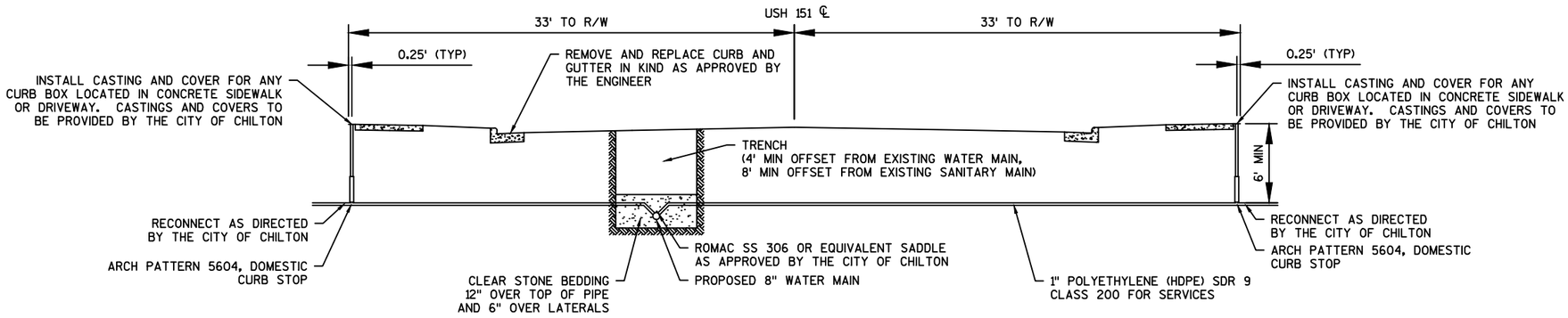


TYPICAL TRENCH RESTORATION DETAIL

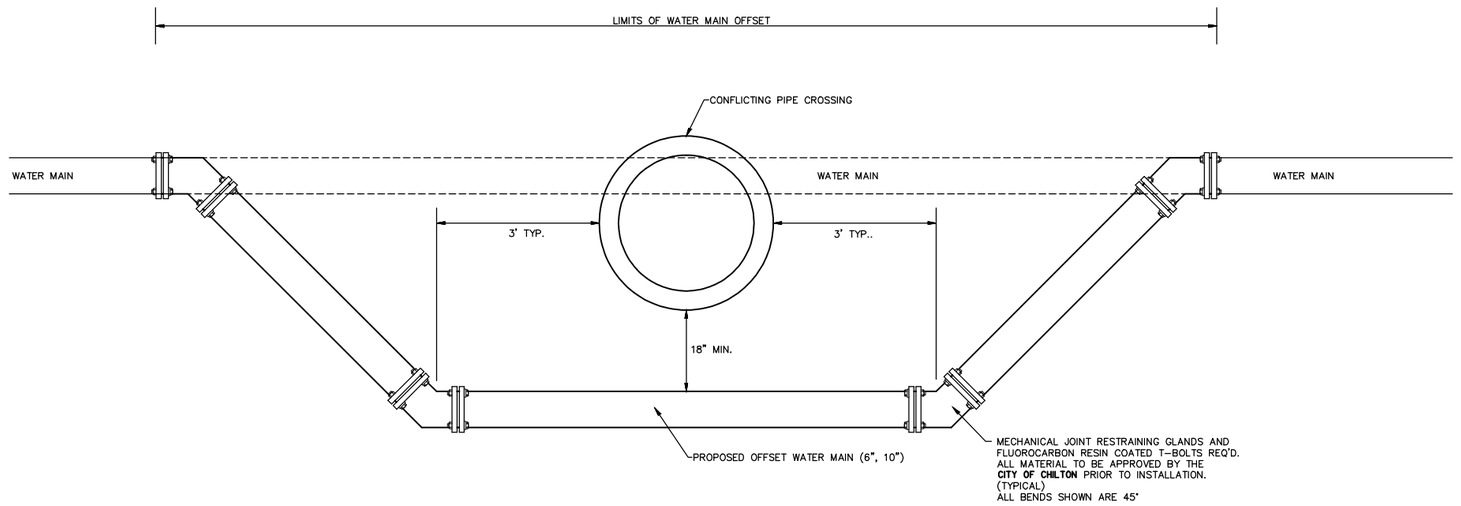
STA. 218+69.96 TO STA. 221+20.92

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CONSTRUCTION DETAILS	SHEET 11	E

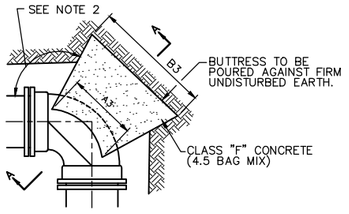
2



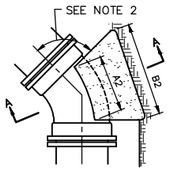
TYPICAL WATER SERVICE CONNECTION DETAIL



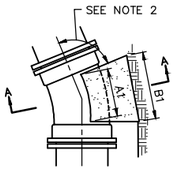
DETAIL FOR WATER MAIN OFFSET



PLAN – 90° BEND



PLAN – 45° BEND



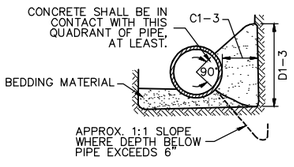
PLAN – 22-1/2° BEND

NOTES:

1. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF 150 P.S.I. AND ON EARTH RESISTANCE OF 2 TONS PER SQ. FT.
2. DIMENSION C1–3 SHOULD BE LARGE ENOUGH TO MAKE ANGLE EQUAL TO OR LARGER THAN 45°.
3. DIMENSION A1–3 SHOULD BE AS LARGE AS POSSIBLE WITHOUT INTERFERING WITH M.J. BOLTS.
4. SHAPE OF BACK OF BUTTRESS MAY VARY AS LONG AS POUR IS AGAINST FIRM UNDISTURBED EARTH.
5. ALL IRON PIPE AND FITTINGS SHALL BE WRAPPED IN POLYETHYLENE.

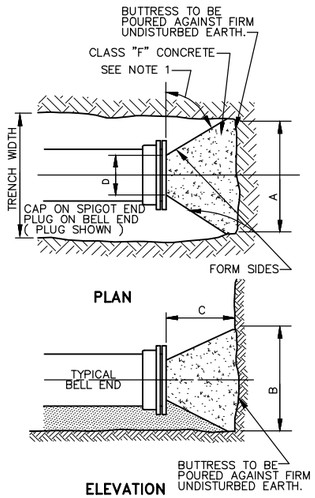
BUTTRESS DIMENSIONS

PIPE SIZE	22-1/2° BENDS		45° BENDS		90° BENDS	
	B1	D1	B2	D2	B3	D3
6"	1'–0"	1'–0"	1'–0"	1'–0"	1'–4"	1'–2"
8"	1'–0"	1'–0"	1'–4"	1'–2"	1'–10"	1'–6"
12"	1'–4"	1'–4"	1'–10"	1'–10"	2'–0"	2'–3"
16"	1'–10"	1'–8"	2'–6"	2'–4"	3'–10"	2'–10"
20"	2'–4"	2'–0"	3'–3"	2'–10"	3'–0"	3'–4"
24"	2'–10"	2'–4"	4'–0"	3'–3"	6'–4"	3'–10"
30"	3'–6"	3'–0"	5'–4"	3'–10"	8'–0"	4'–8"



SECTION A–A

BLOCKING FOR BENDS



ELEVATION

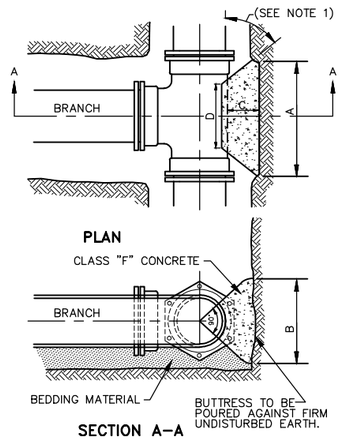
BUTTRESS DIMENSIONS

DIA.	A	B	C	D
6"	1'–6"	1'–2"	SEE NOTE 2	SEE NOTE 3
8"	2'–0"	1'–4"		
12"	2'–5"	1'–10"		
16"	3'–4"	2'–4"		
20"	4'–3"	2'–10"		
24"	5'–2"	3'–4"		
30"	6'–9"	4'–0"		

NOTES:

1. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF 150 P.S.I. AND ON EARTH RESISTANCE OF 2 TONS PER SQ. FT.
2. DIMENSION C SHOULD BE LARGE ENOUGH TO MAKE ANGLE EQUAL TO OR LARGER THAN 45°.
3. DIMENSION D SHOULD BE AS LARGE AS POSSIBLE WITHOUT INTERFERING WITH M.J. BOLTS.
4. SHAPE OF BACK OF BUTTRESS MAY VARY AS LONG AS POUR IS AGAINST FIRM UNDISTURBED EARTH.
5. ALL IRON PIPE AND FITTINGS SHALL BE WRAPPED IN POLYETHYLENE.

BLOCKING FOR PLUGS



SECTION A–A

BUTTRESS DIMENSIONS

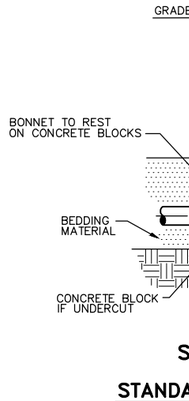
B.D.	A	B	C	D
6"	1'–3"	1'–0"	SEE NOTE 1	SEE NOTE 2
8"	1'–6"	1'–4"		
12"	2'–3"	2'–0"		
16"	3'–2"	2'–6"		
20"	4'–0"	3'–0"		
24"	5'–3"	3'–4"		
30"	6'–6"	4'–3"		

B.D. = BRANCH DIAMETER

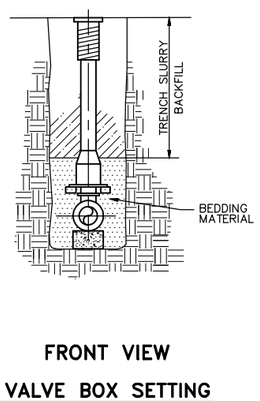
NOTES:

1. DIMENSION "C" SHOULD BE LARGE ENOUGH TO MAKE ANGLE EQUAL TO OR LARGER THAN 45°.
2. DIMENSION "D" EQUALS APPROX. I.D. OF PIPE LESS 2". AN EFFORT SHOULD BE MADE TO PREVENT THE CONCRETE FROM COVERING THE M.J. BOLTS.
3. WHERE BUTTRESES ARE NOT POSSIBLE BECAUSE OF POOR SOIL CONDITIONS OR LACK OF ROOM, STRAPPING SHALL BE PERMITTED.
4. DIMENSIONS IN TABLE ARE BASED ON A WATER PRESSURE OF 150 P.S.I. AND ON EARTH RESISTANCE OF 2 TONS PER SQ. FT.
5. ALL IRON PIPE AND FITTINGS SHALL BE WRAPPED IN POLYETHYLENE.

BLOCKING FOR TEES

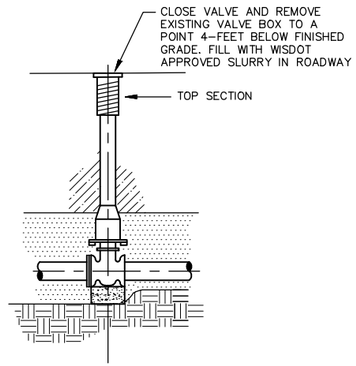


SIDE VIEW



FRONT VIEW

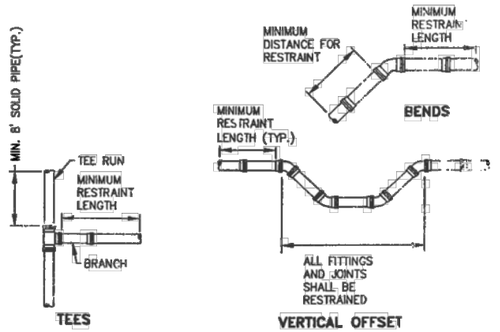
STANDARD VALVE & VALVE BOX SETTING



VALVE ABANDONMENT DETAIL

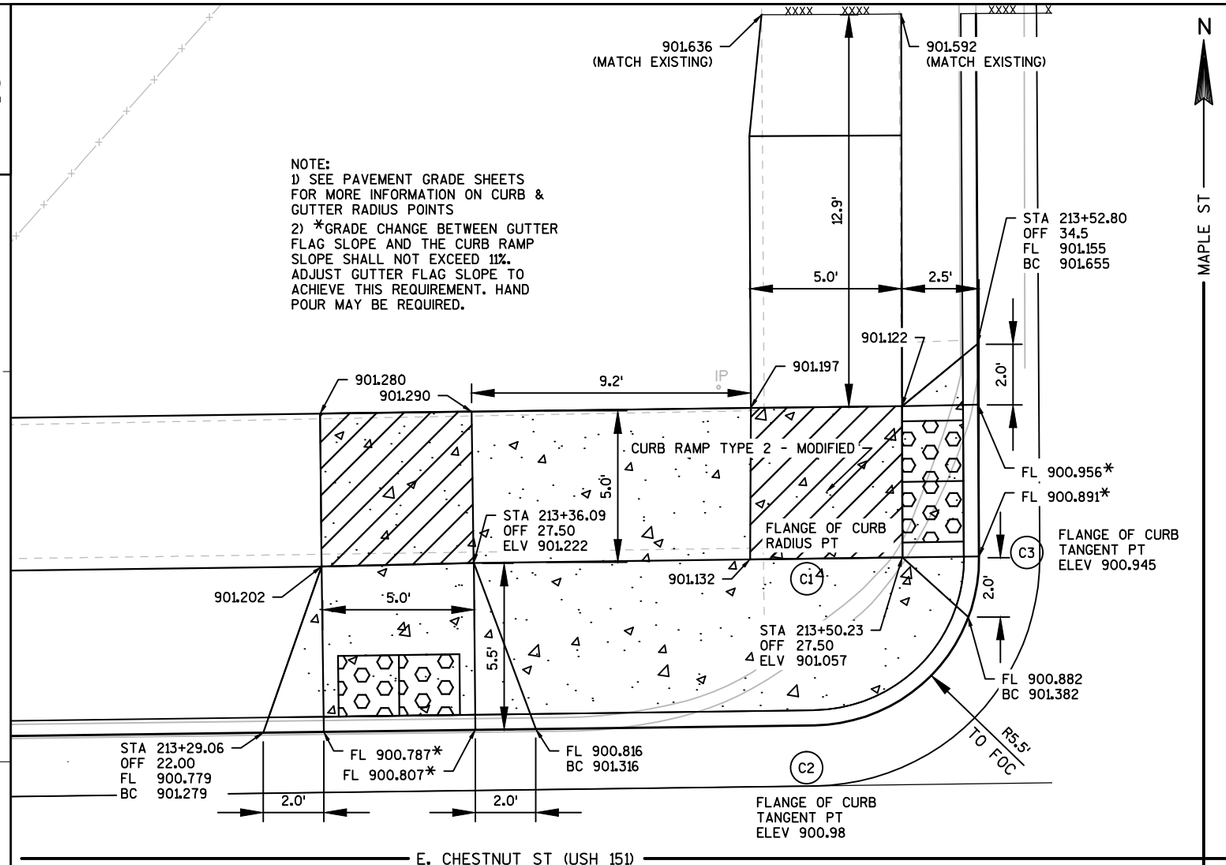
Minimum Restraint Length (ft) on both sides of the Fitting

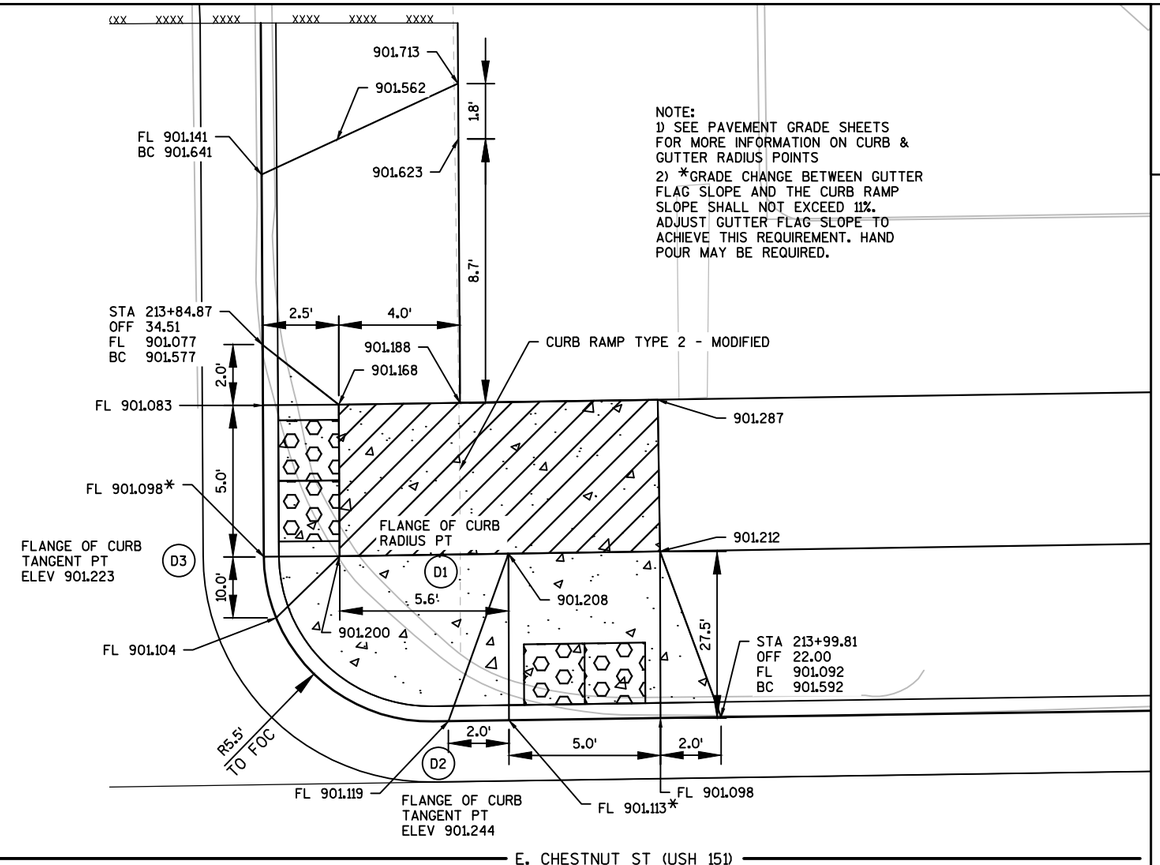
Fitting Type/Nominal Size	6"	8"	12"	16"
11 1/4° Bend	2	2	3	3
22 1/2° Bend	3	3	5	6
45° Bend	5	6	9	11
90° Bend	11	15	21	27
Dead End	30	40	56	73
Top Side of a Vertical Offset	13	17	24	31
Tee Run x Branch 6"BY	14			
Tee Run x Branch 8"BY	10	24		
Tee Run x Branch 12"BY	1	15	40	
Tee Run x Branch 16"BY	1	7	33	56

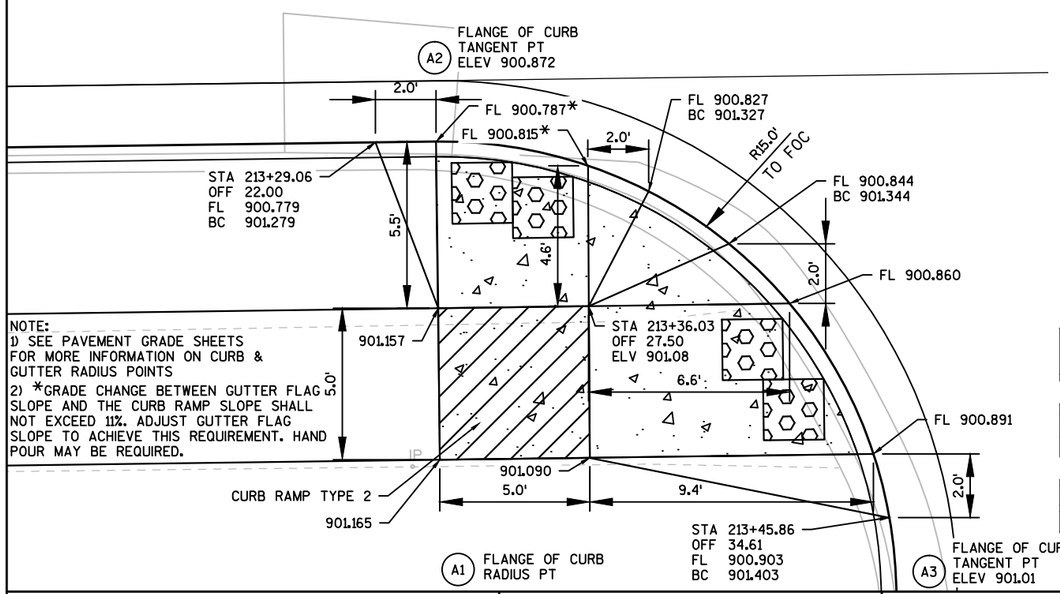


– USE EBBA 1100 HD SPLIT HARNESS ON EXISTING PUSH–ON PIPE BELLS
– USE EBBA 1100 MEGALUGS ON NEW MECHANICAL JOINTS.

WATER MAIN RESTRAINT DETAIL





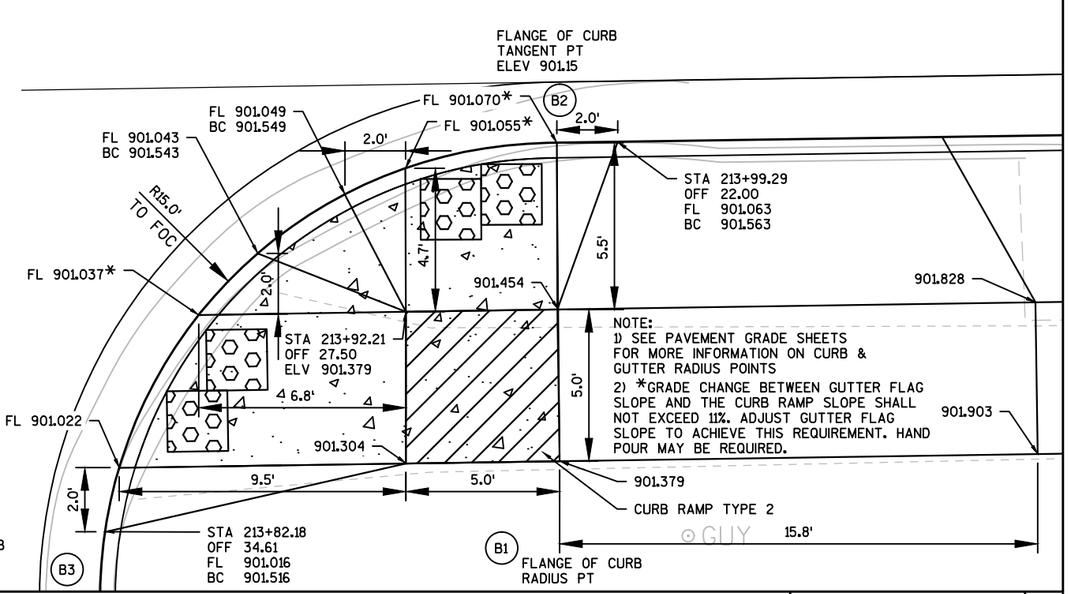


CONCRETE SIDEWALK 7-INCH

LEVEL LANDING AREA

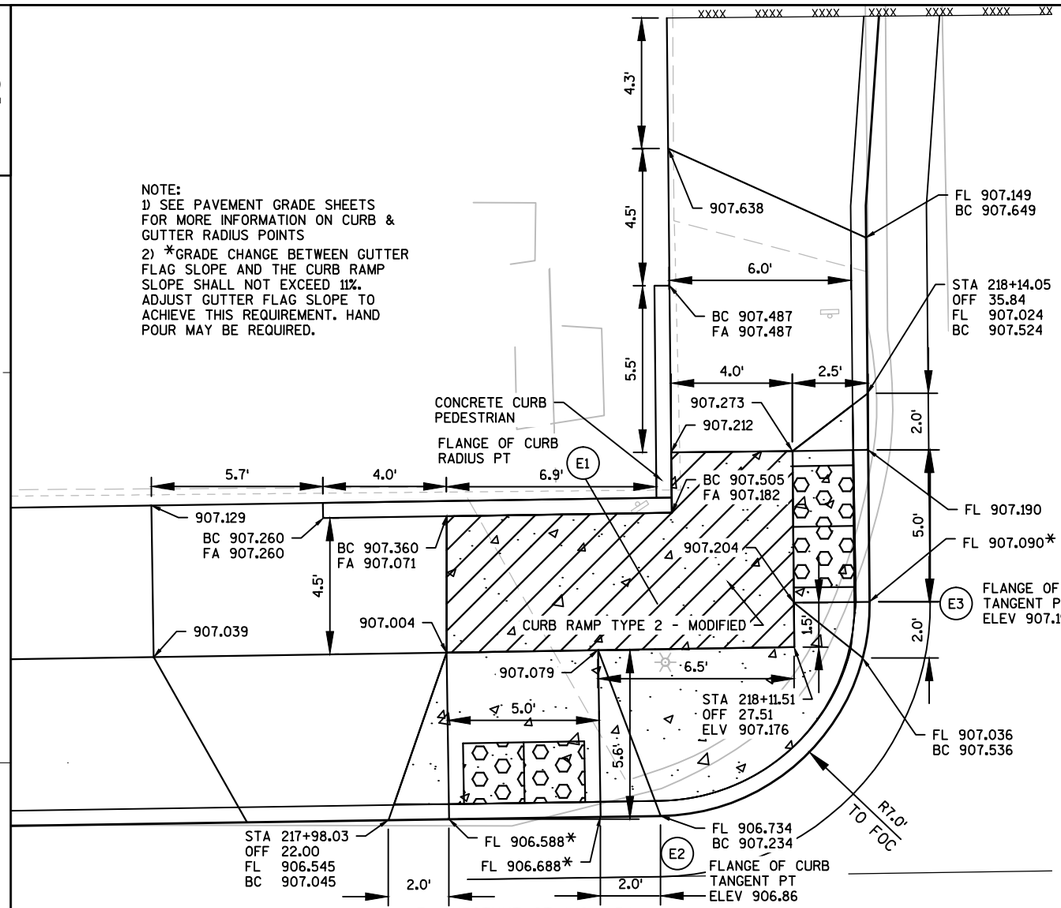
CURB RAMP DETECTABLE WARNING FIELD YELLOW

FLANGE OF CURB TANGENT PT ELEV 901.01



PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CONTRUCTION DETAILS - CURB RAMPS	SHEET 14	E

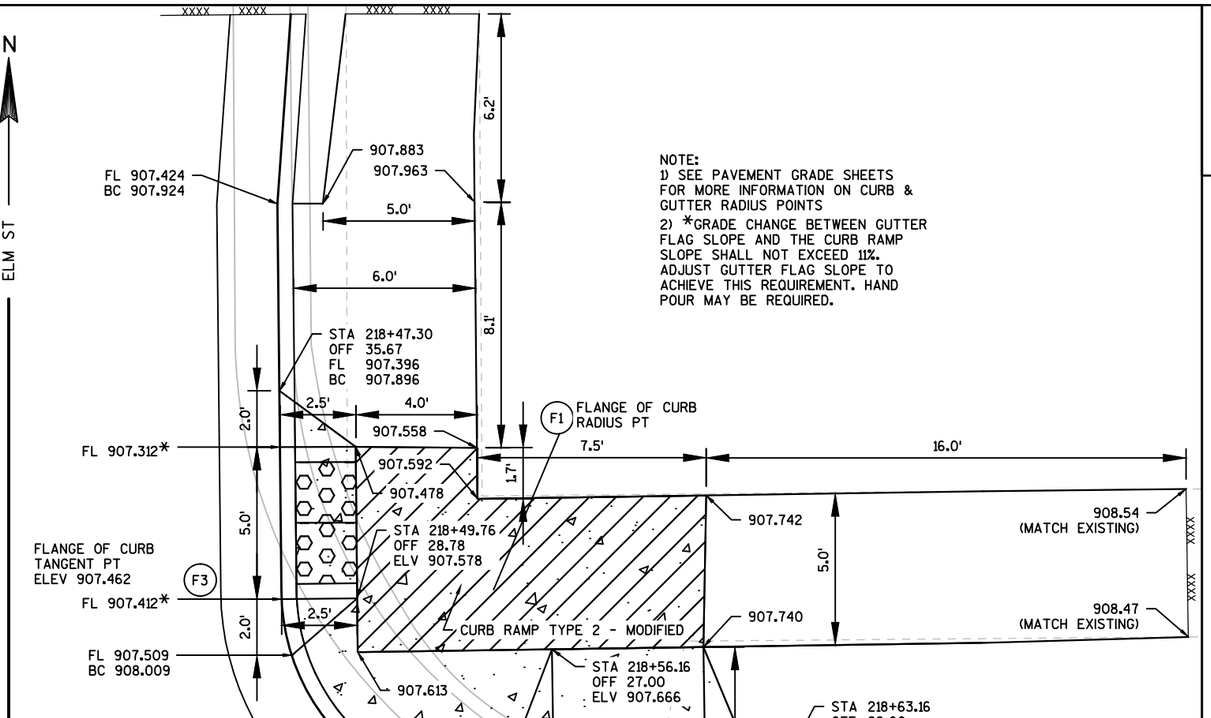
NOTE:
1) SEE PAVEMENT GRADE SHEETS FOR MORE INFORMATION ON CURB & GUTTER RADIUS POINTS
2) *GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. ADJUST GUTTER FLAG SLOPE TO ACHIEVE THIS REQUIREMENT. HAND POUR MAY BE REQUIRED.



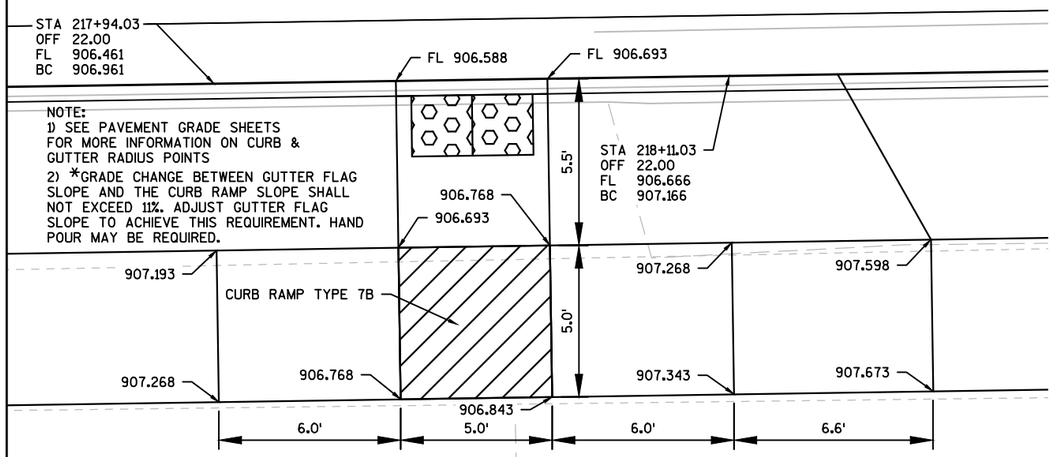
N

ELM ST

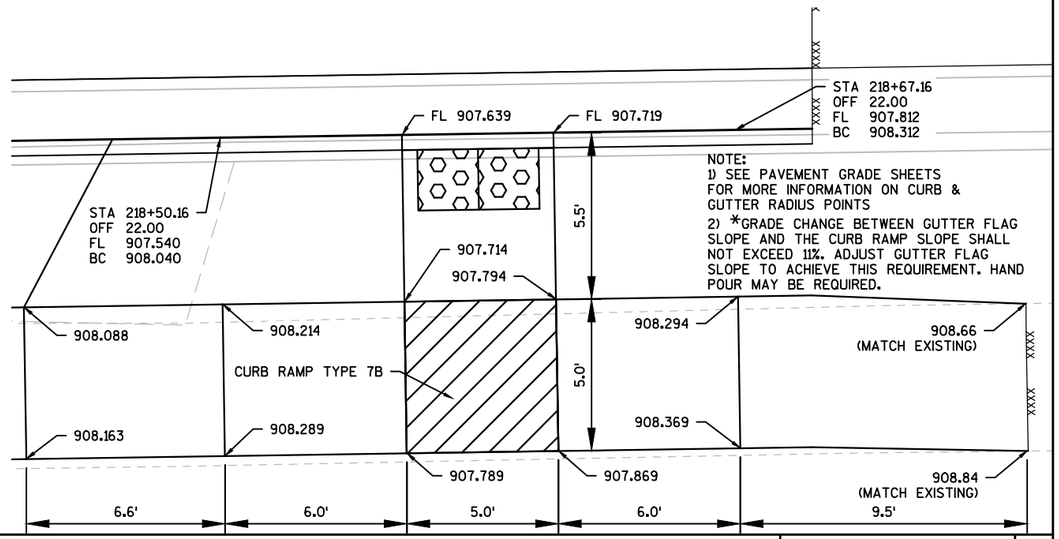
NOTE:
1) SEE PAVEMENT GRADE SHEETS FOR MORE INFORMATION ON CURB & GUTTER RADIUS POINTS
2) *GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. ADJUST GUTTER FLAG SLOPE TO ACHIEVE THIS REQUIREMENT. HAND POUR MAY BE REQUIRED.



NOTE:
1) SEE PAVEMENT GRADE SHEETS FOR MORE INFORMATION ON CURB & GUTTER RADIUS POINTS
2) *GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. ADJUST GUTTER FLAG SLOPE TO ACHIEVE THIS REQUIREMENT. HAND POUR MAY BE REQUIRED.



NOTE:
1) SEE PAVEMENT GRADE SHEETS FOR MORE INFORMATION ON CURB & GUTTER RADIUS POINTS
2) *GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. ADJUST GUTTER FLAG SLOPE TO ACHIEVE THIS REQUIREMENT. HAND POUR MAY BE REQUIRED.



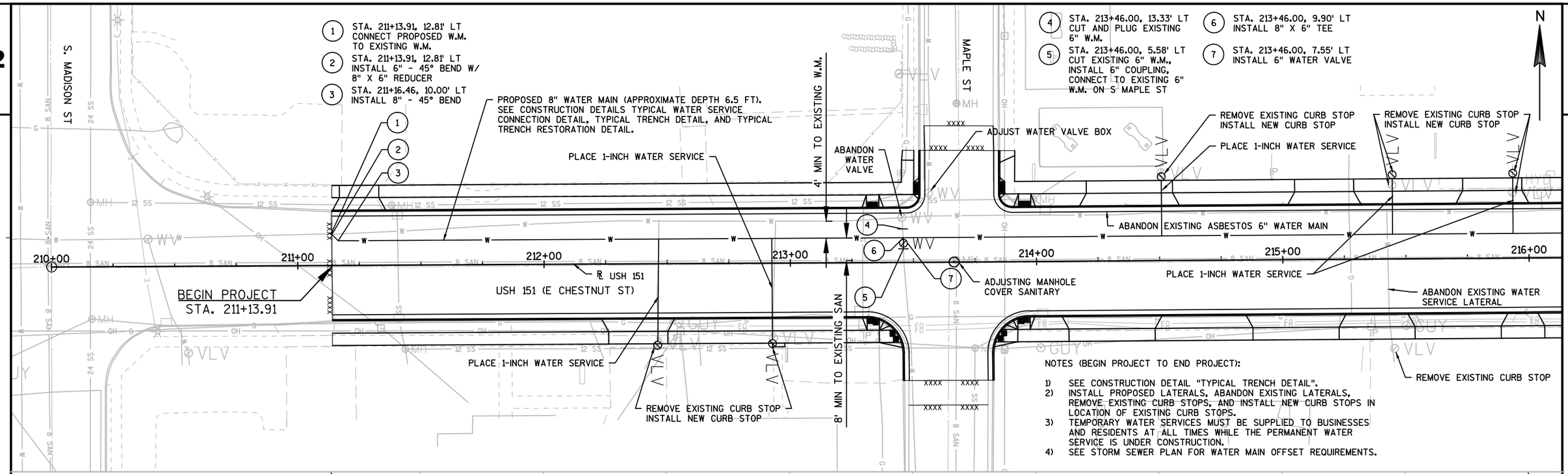
CONCRETE SIDEWALK 7-INCH

LEVEL LANDING AREA

CURB RAMP DETECTABLE WARNING FIELD YELLOW

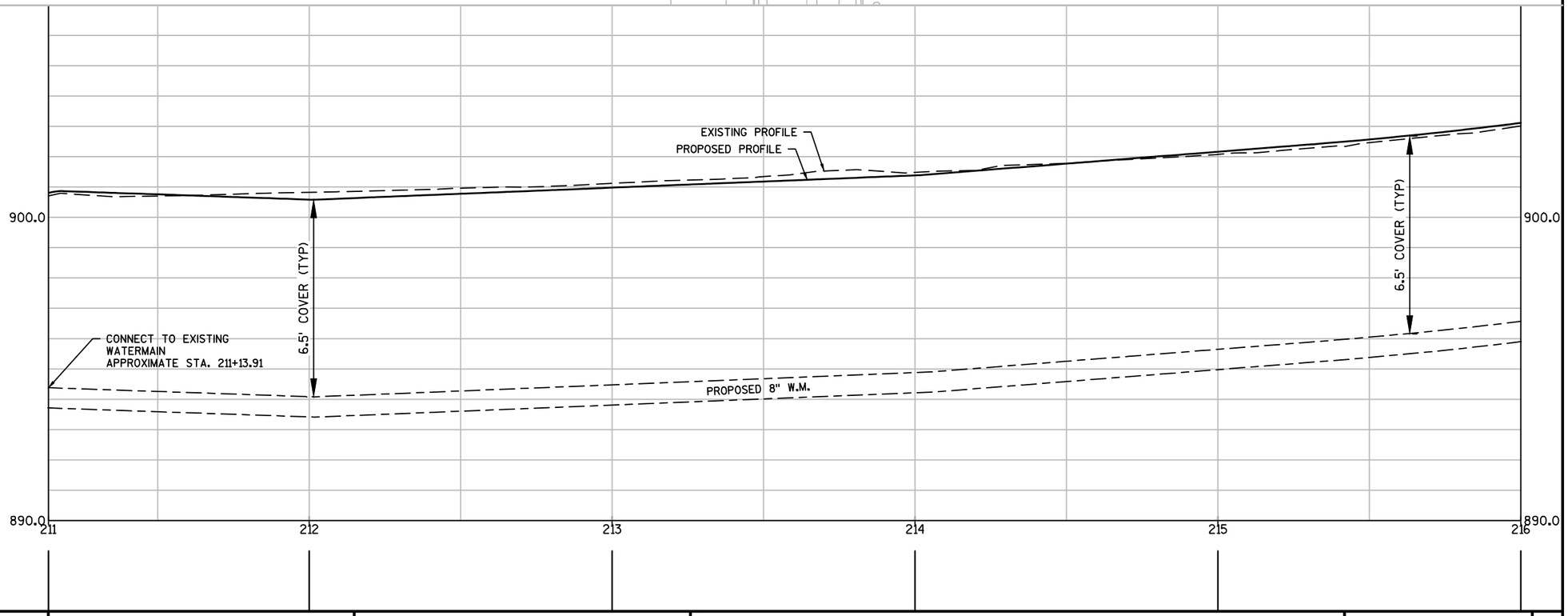
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CONSTRUCTION DETAILS - CURB RAMPS	SHEET 15	E

1. STA. 211+13.91, 12.81' LT CONNECT PROPOSED W.M. TO EXISTING W.M.
2. STA. 211+13.91, 12.81' LT INSTALL 6" - 45° BEND W/ 8" X 6" REDUCER
3. STA. 211+16.46, 10.00' LT INSTALL 8" - 45° BEND
4. STA. 213+46.00, 13.33' LT CUT AND PLUG EXISTING 6" W.M.
5. STA. 213+46.00, 5.58' LT CUT EXISTING 6" W.M., INSTALL 6" COUPLING, CONNECT TO EXISTING 6" W.M. ON S MAPLE ST
6. STA. 213+46.00, 9.90' LT INSTALL 8" X 6" TEE
7. STA. 213+46.00, 7.55' LT INSTALL 6" WATER VALVE



NOTES (BEGIN PROJECT TO END PROJECT):

1) SEE CONSTRUCTION DETAIL "TYPICAL TRENCH DETAIL".
2) INSTALL PROPOSED LATERALS, ABANDON EXISTING LATERALS, REMOVE EXISTING CURB STOPS, AND INSTALL NEW CURB STOPS IN LOCATION OF EXISTING CURB STOPS.
3) TEMPORARY WATER SERVICES MUST BE SUPPLIED TO BUSINESSES AND RESIDENTS AT ALL TIMES WHILE THE PERMANENT WATER SERVICE IS UNDER CONSTRUCTION.
4) SEE STORM SEWER PLAN FOR WATER MAIN OFFSET REQUIREMENTS.

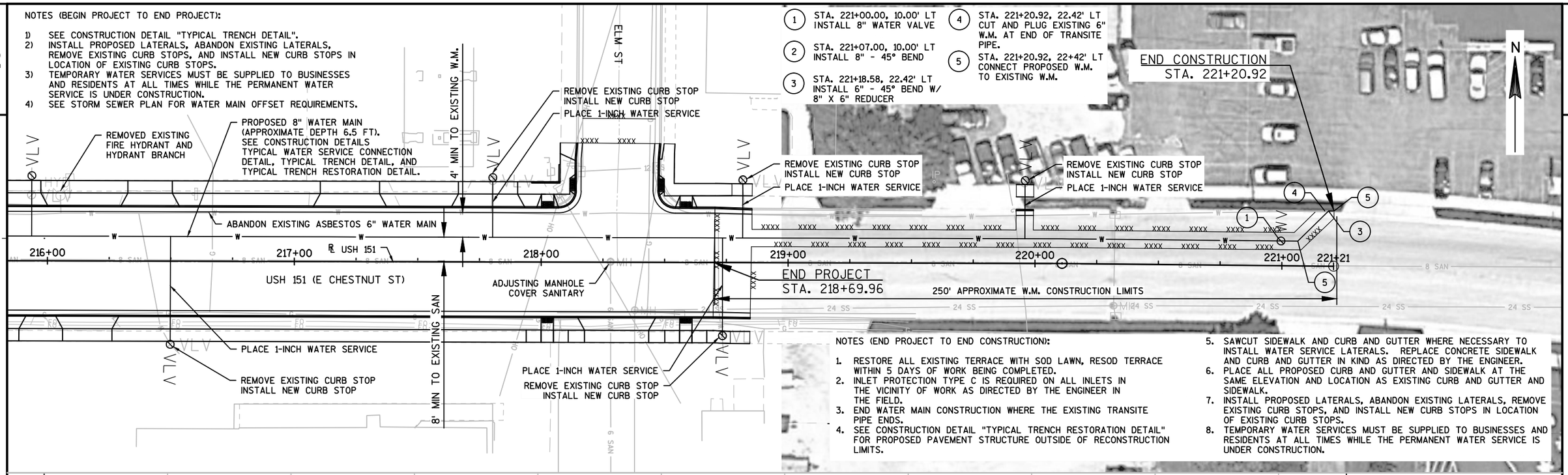


PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PLAN DETAILS: WATERMAIN REPLACEMENT	SHEET 16 E

NOTES (BEGIN PROJECT TO END PROJECT):

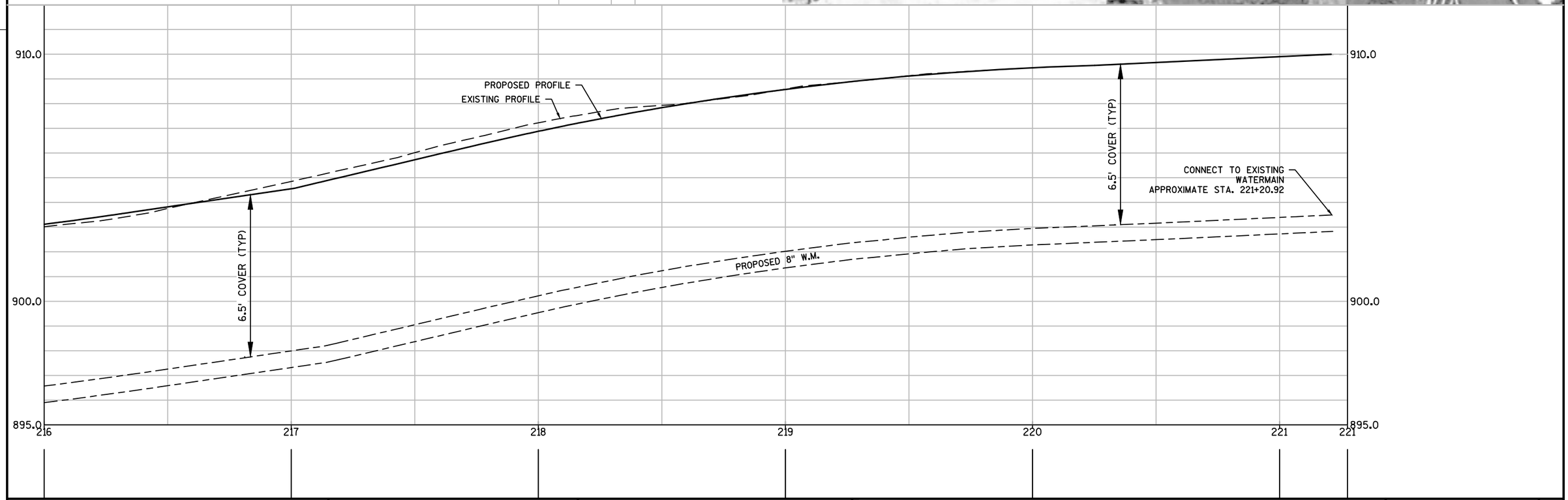
1) SEE CONSTRUCTION DETAIL "TYPICAL TRENCH DETAIL".
2) INSTALL PROPOSED LATERALS, ABANDON EXISTING LATERALS, REMOVE EXISTING CURB STOPS, AND INSTALL NEW CURB STOPS IN LOCATION OF EXISTING CURB STOPS.
3) TEMPORARY WATER SERVICES MUST BE SUPPLIED TO BUSINESSES AND RESIDENTS AT ALL TIMES WHILE THE PERMANENT WATER SERVICE IS UNDER CONSTRUCTION.
4) SEE STORM SEWER PLAN FOR WATER MAIN OFFSET REQUIREMENTS.

1. STA. 221+00.00, 10.00' LT INSTALL 8" WATER VALVE
2. STA. 221+07.00, 10.00' LT INSTALL 8" - 45° BEND
3. STA. 221+18.58, 22.42' LT INSTALL 6" - 45° BEND W/ 8" X 6" REDUCER
4. STA. 221+20.92, 22.42' LT CUT AND PLUG EXISTING 6" W.M. AT END OF TRANSITE PIPE.
5. STA. 221+20.92, 22+42' LT CONNECT PROPOSED W.M. TO EXISTING W.M.



NOTES (END PROJECT TO END CONSTRUCTION):

1. RESTORE ALL EXISTING TERRACE WITH SOD LAWN, RESOD TERRACE WITHIN 5 DAYS OF WORK BEING COMPLETED.
2. INLET PROTECTION TYPE C IS REQUIRED ON ALL INLETS IN THE VICINITY OF WORK AS DIRECTED BY THE ENGINEER IN THE FIELD.
3. END WATER MAIN CONSTRUCTION WHERE THE EXISTING TRANSITE PIPE ENDS.
4. SEE CONSTRUCTION DETAIL "TYPICAL TRENCH RESTORATION DETAIL" FOR PROPOSED PAVEMENT STRUCTURE OUTSIDE OF RECONSTRUCTION LIMITS.
5. SAWCUT SIDEWALK AND CURB AND GUTTER WHERE NECESSARY TO INSTALL WATER SERVICE LATERALS. REPLACE CONCRETE SIDEWALK AND CURB AND GUTTER IN KIND AS DIRECTED BY THE ENGINEER.
6. PLACE ALL PROPOSED CURB AND GUTTER AND SIDEWALK AT THE SAME ELEVATION AND LOCATION AS EXISTING CURB AND GUTTER AND SIDEWALK.
7. INSTALL PROPOSED LATERALS, ABANDON EXISTING LATERALS, REMOVE EXISTING CURB STOPS, AND INSTALL NEW CURB STOPS IN LOCATION OF EXISTING CURB STOPS.
8. TEMPORARY WATER SERVICES MUST BE SUPPLIED TO BUSINESSES AND RESIDENTS AT ALL TIMES WHILE THE PERMANENT WATER SERVICE IS UNDER CONSTRUCTION.

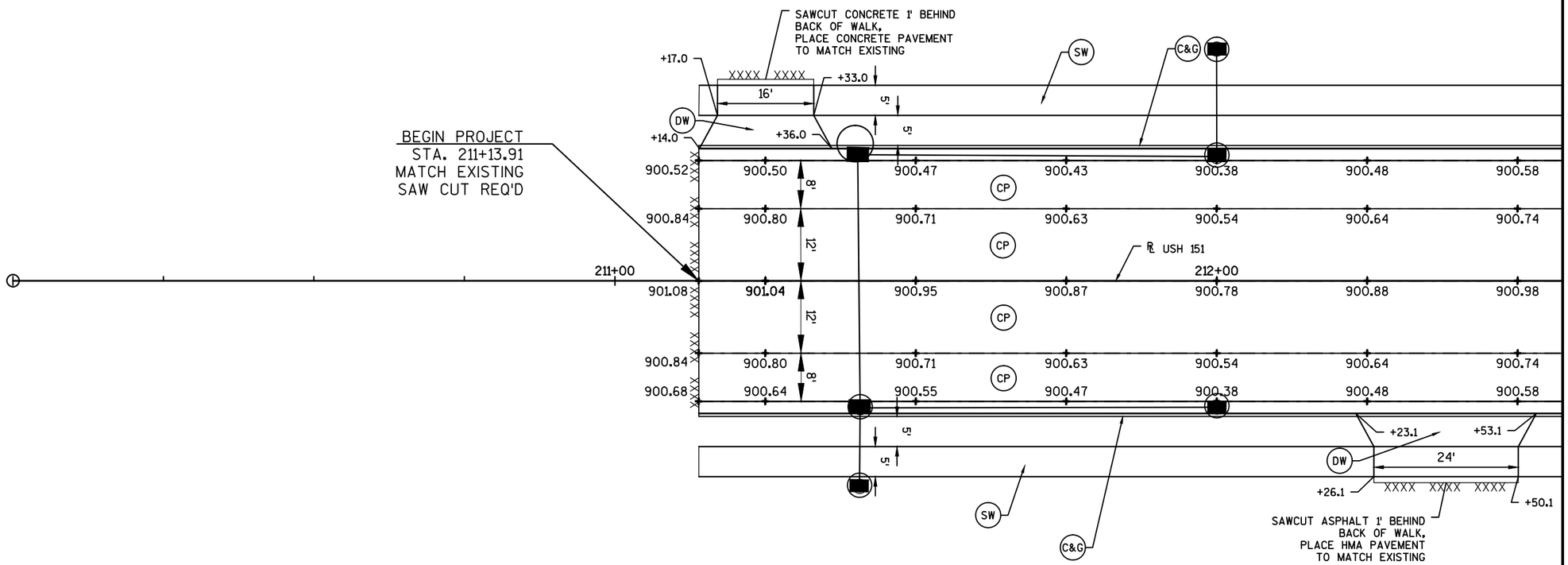


PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PLAN DETAILS: WATERMAIN REPLACEMENT	SHEET 17 E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\021201_PD.DWG
LAYOUT NAME - 021202_PD
PLOT DATE : 4/22/2015 10:00 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:40_XREF
WISDOT/CADDS SHEET 41

2

N



(CP) CONCRETE PAVEMENT 8-INCH

(SW) CONCRETE SIDEWALK 5-INCH

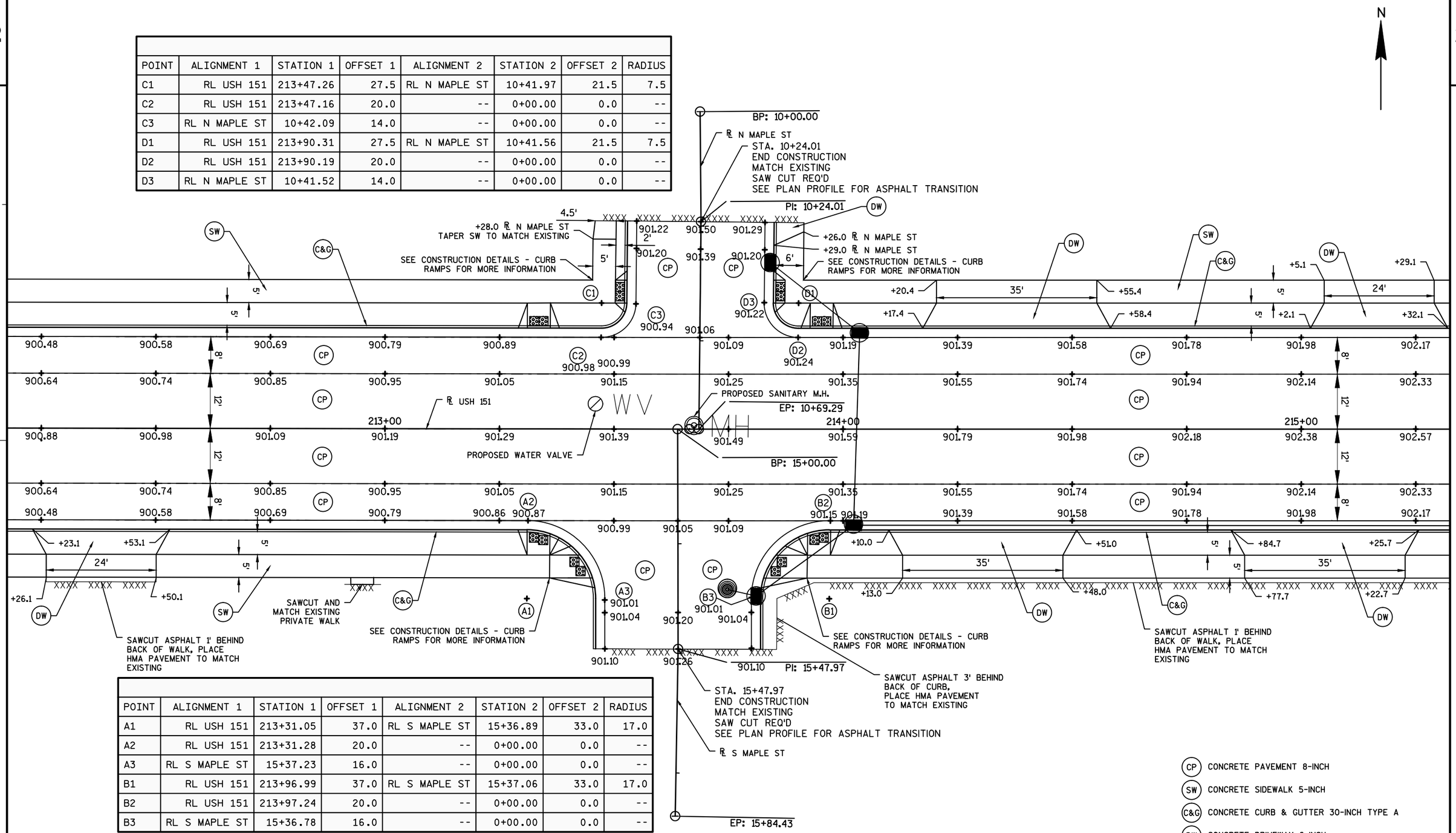
(C&G) CONCRETE CURB & GUTTER 30-INCH TYPE A

(DW) CONCRETE DRIVEWAY 6-INCH

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PAVEMENT GRADES	SHEET 18 E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\021301_JD.DWG
LAYOUT NAME - 021301_JD
PLOT DATE : 4/22/2015 10:01 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:20_XREF
WISDOT/CADDS SHEET 42

POINT	ALIGNMENT 1	STATION 1	OFFSET 1	ALIGNMENT 2	STATION 2	OFFSET 2	RADIUS
C1	RL USH 151	213+47.26	27.5	RL N MAPLE ST	10+41.97	21.5	7.5
C2	RL USH 151	213+47.16	20.0	--	0+00.00	0.0	--
C3	RL N MAPLE ST	10+42.09	14.0	--	0+00.00	0.0	--
D1	RL USH 151	213+90.31	27.5	RL N MAPLE ST	10+41.56	21.5	7.5
D2	RL USH 151	213+90.19	20.0	--	0+00.00	0.0	--
D3	RL N MAPLE ST	10+41.52	14.0	--	0+00.00	0.0	--



POINT	ALIGNMENT 1	STATION 1	OFFSET 1	ALIGNMENT 2	STATION 2	OFFSET 2	RADIUS
A1	RL USH 151	213+31.05	37.0	RL S MAPLE ST	15+36.89	33.0	17.0
A2	RL USH 151	213+31.28	20.0	--	0+00.00	0.0	--
A3	RL S MAPLE ST	15+37.23	16.0	--	0+00.00	0.0	--
B1	RL USH 151	213+96.99	37.0	RL S MAPLE ST	15+37.06	33.0	17.0
B2	RL USH 151	213+97.24	20.0	--	0+00.00	0.0	--
B3	RL S MAPLE ST	15+36.78	16.0	--	0+00.00	0.0	--

- CP CONCRETE PAVEMENT 8-INCH
- SW CONCRETE SIDEWALK 5-INCH
- C&G CONCRETE CURB & GUTTER 30-INCH TYPE A
- DW CONCRETE DRIVEWAY 6-INCH

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PAVEMENT GRADES	SHEET 19	E

2

N

SAWCUT CONCRETE 1' BEHIND BACK OF WALK, PLACE CONCRETE TO MATCH EXISTING

SAWCUT CONCRETE 2' BEHIND BACK OF WALK, PLACE CONCRETE TO MATCH EXISTING

SAWCUT ASPHALT 2' BEHIND BACK OF WALK, PLACE HMA PAVEMENT TO MATCH EXISTING

END SAWCUT ASPHALT BEGIN SAWCUT CONCRETE

DW +5.1 5' 5' +2.1 24' +29.1 +32.1

SW

DW +63.6 +73.2 +70.2 24' +97.2 +0.2

DW +32.5 +29.5 18' +50.5 +53.5

C&G +10.4 +7.4

DW +44.5 +34.4 +37.4 24'

DW +66.4 +63.4 5' 24'

901.78	901.98	902.17	902.39	902.64	902.94	903.28	903.64	904.01	CP	904.38	904.97	905.56	906.16
901.94	902.14	902.33	902.55	902.80	903.10	903.44	903.80	904.17	CP	904.54	905.13	905.72	906.32
215+00					216+00				CP	217+00			
902.18	902.38	902.57	902.79	903.04	903.34	903.68	904.04	904.41	CP	904.78	905.37	905.96	906.56
901.94	902.14	902.33	902.55	902.80	903.10	903.44	903.80	904.17	CP	904.54	905.13	905.72	906.32
901.78	901.98	902.17	902.39	902.64	902.94	903.28	903.64	904.01		904.38	904.97	905.56	906.16

8' 12' 12' 8' 8' 12' 12' 8'

℄ USH 151

5' 5' +84.7 +77.7 35' +25.7 +22.7 DW +50.5 +53.5 35' +91.5 +88.5 DW +3.2 +6.2 20' +29.2 +26.2 DW SW C&G 5' 5'

SAWCUT ASPHALT 1' BEHIND BACK OF WALK, PLACE HMA PAVEMENT TO MATCH EXISTING

(CP) CONCRETE PAVEMENT 8-INCH

(SW) CONCRETE SIDEWALK 5-INCH

(C&G) CONCRETE CURB & GUTTER 30-INCH TYPE A

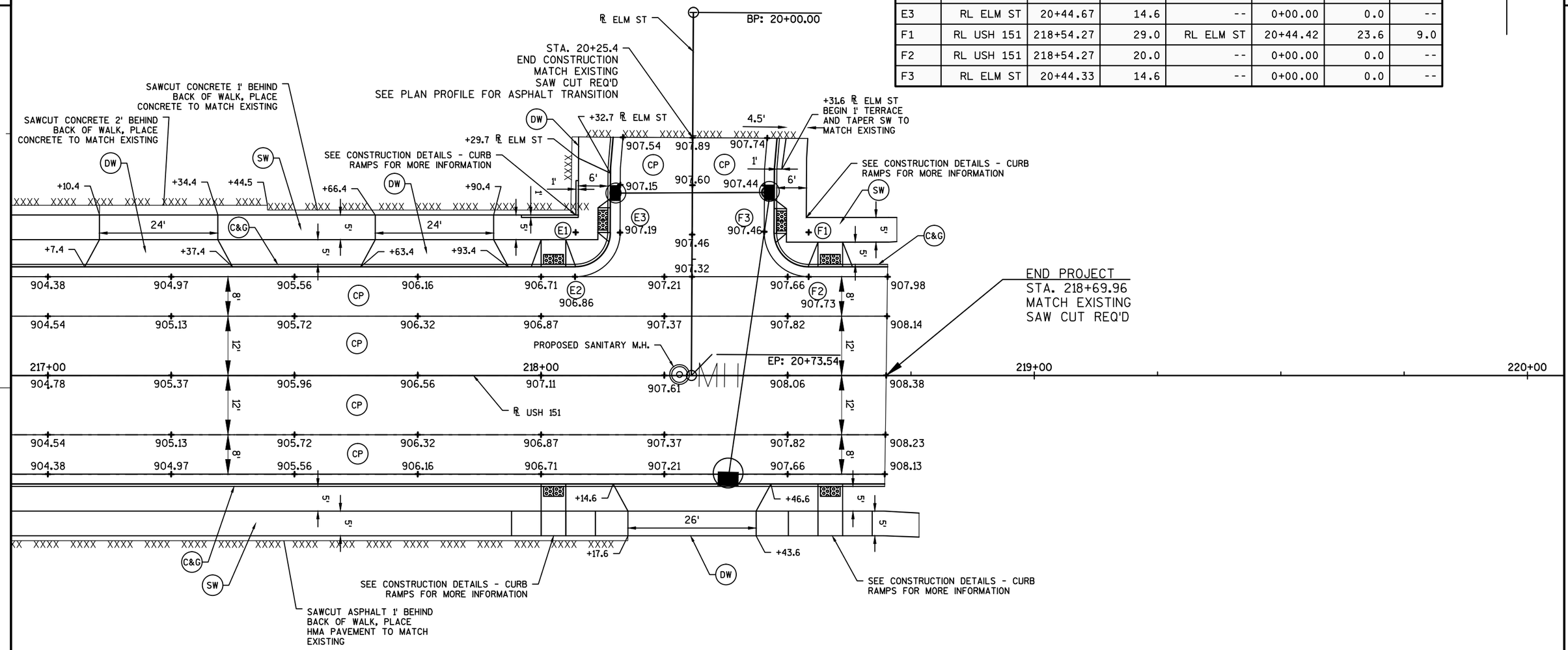
(DW) CONCRETE DRIVEWAY 6-INCH

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PAVEMENT GRADES	SHEET 20	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\021301_JD.DWG
LAYOUT NAME - 021303_JD

PLOT DATE : 4/22/2015 10:01 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:20_XREF

POINT	ALIGNMENT 1	STATION 1	OFFSET 1	ALIGNMENT 2	STATION 2	OFFSET 2	RADIUS
E1	RL USH 151	218+07.02	29.0	RL ELM ST	20+44.67	23.6	9.0
E2	RL USH 151	218+06.88	20.0	--	0+00.00	0.0	--
E3	RL ELM ST	20+44.67	14.6	--	0+00.00	0.0	--
F1	RL USH 151	218+54.27	29.0	RL ELM ST	20+44.42	23.6	9.0
F2	RL USH 151	218+54.27	20.0	--	0+00.00	0.0	--
F3	RL ELM ST	20+44.33	14.6	--	0+00.00	0.0	--



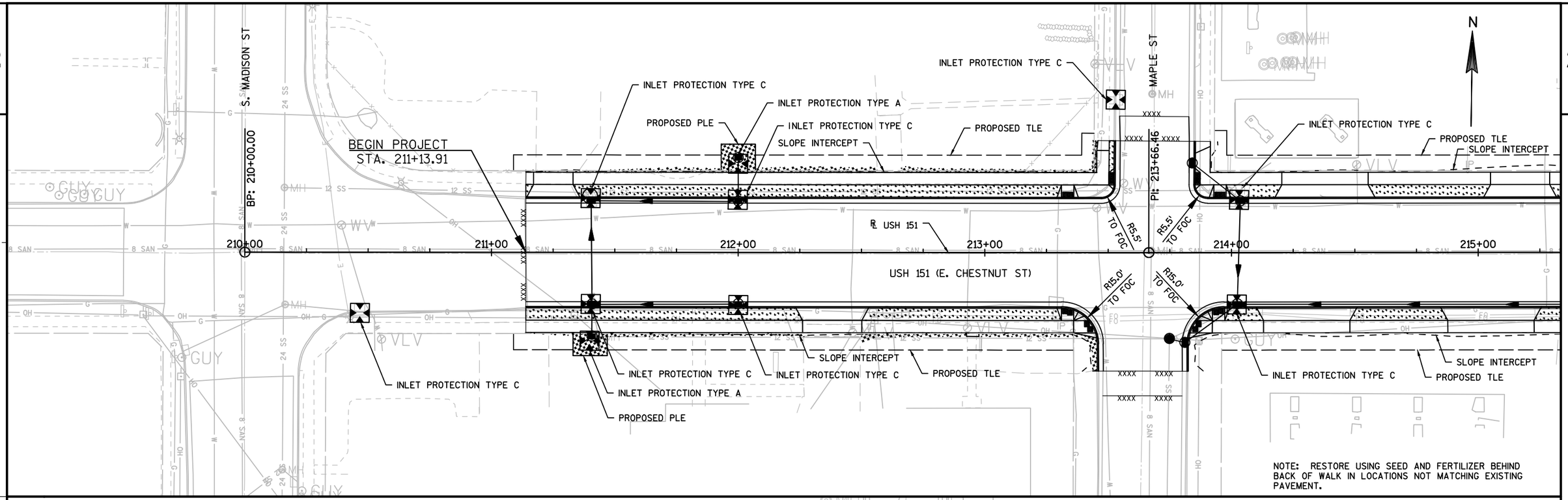
CP CONCRETE PAVEMENT 8-INCH

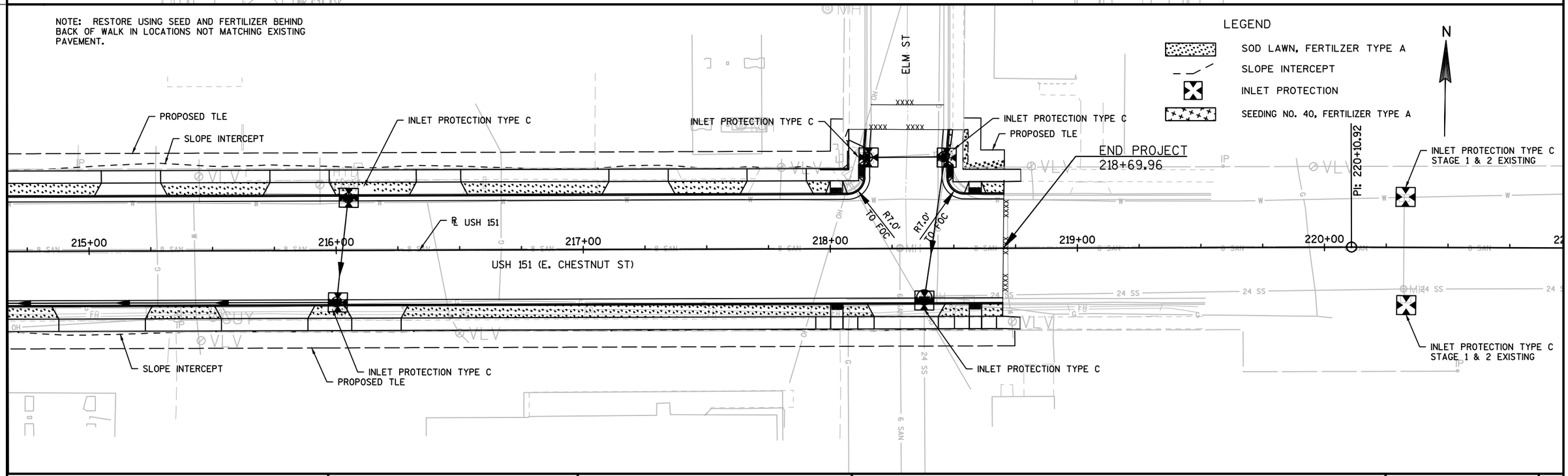
SW CONCRETE SIDEWALK 5-INCH

C&G CONCRETE CURB & GUTTER 30-INCH TYPE A

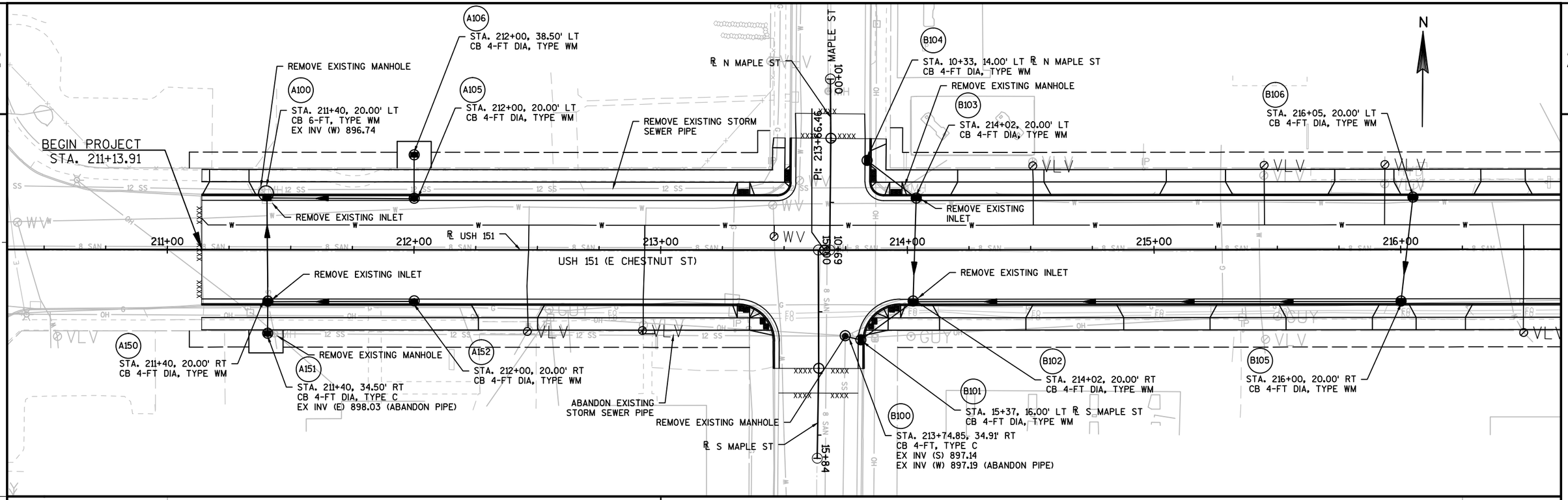
DW CONCRETE DRIVEWAY 6-INCH

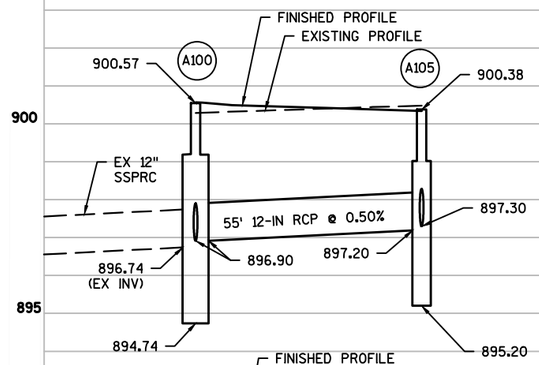
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PAVEMENT GRADES	SHEET 21 E

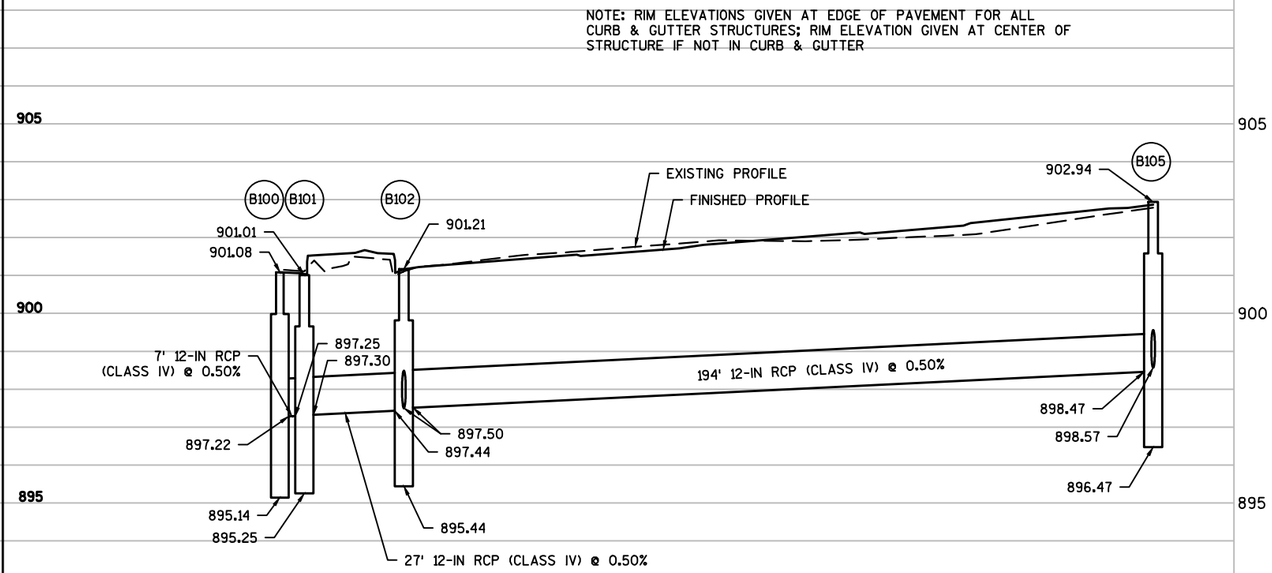




PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	EROSION CONTROL STAGE 1 & 2	SHEET 22	E

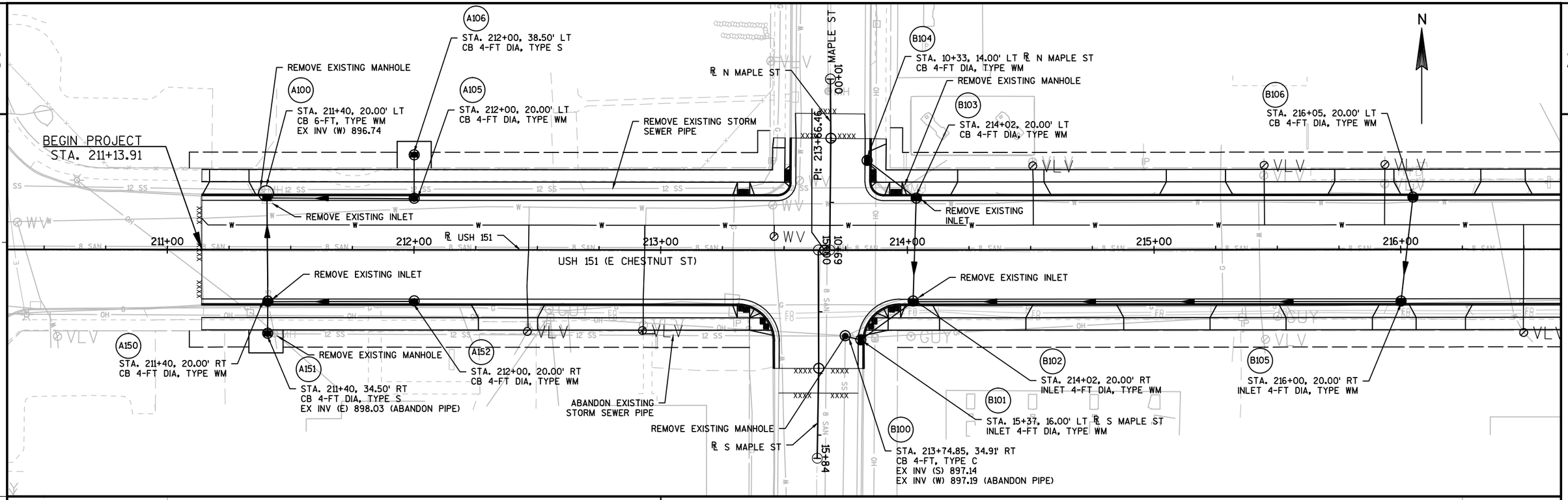


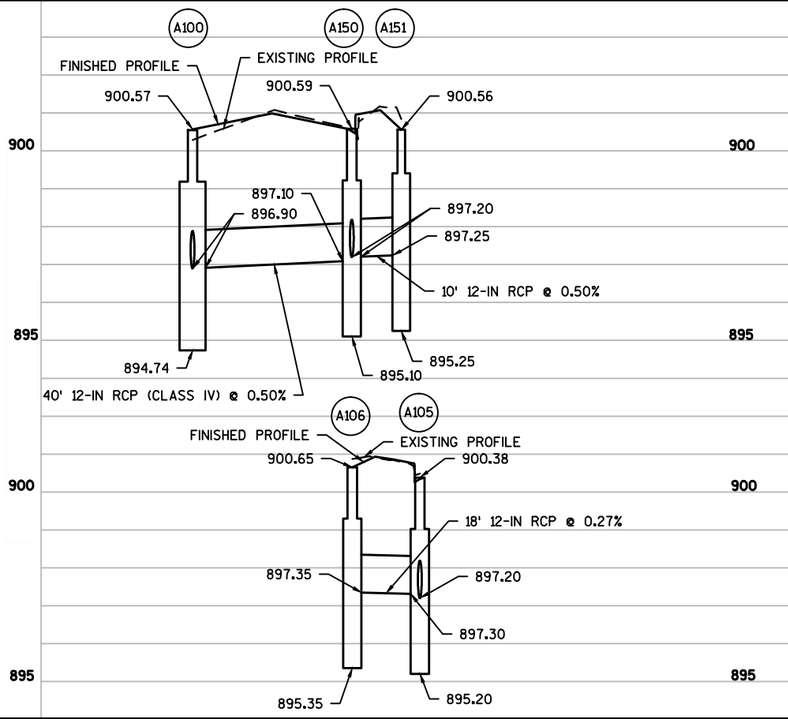




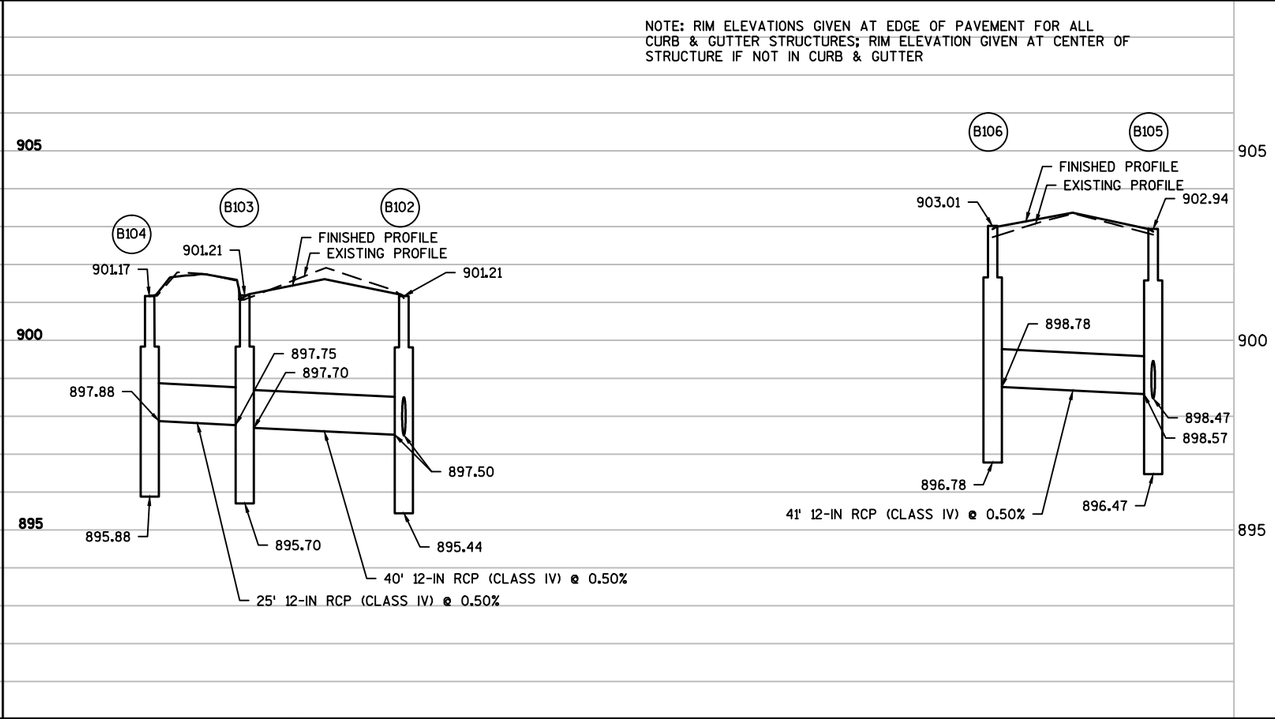
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	STORM SEWER: USH 151	SHEET 23 E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\022501_SS.DWG
LAYOUT NAME - 050101_SS
PLOT DATE : 4/22/2015 10:02 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:40_XREF
WISDOT/CADDS SHEET 44



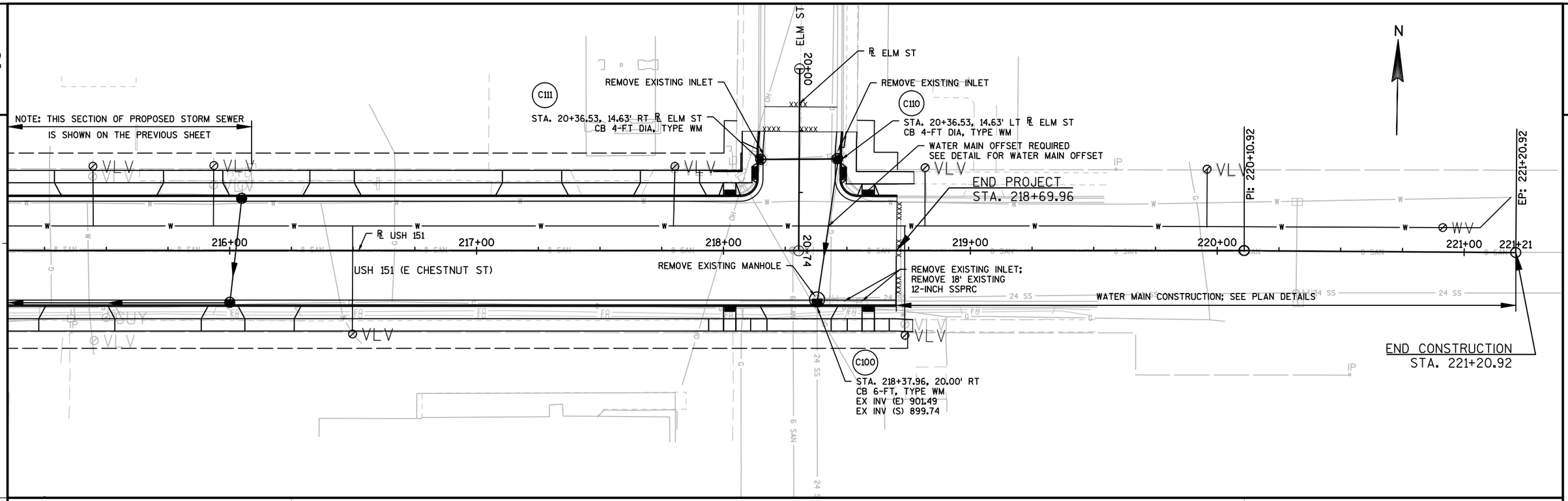


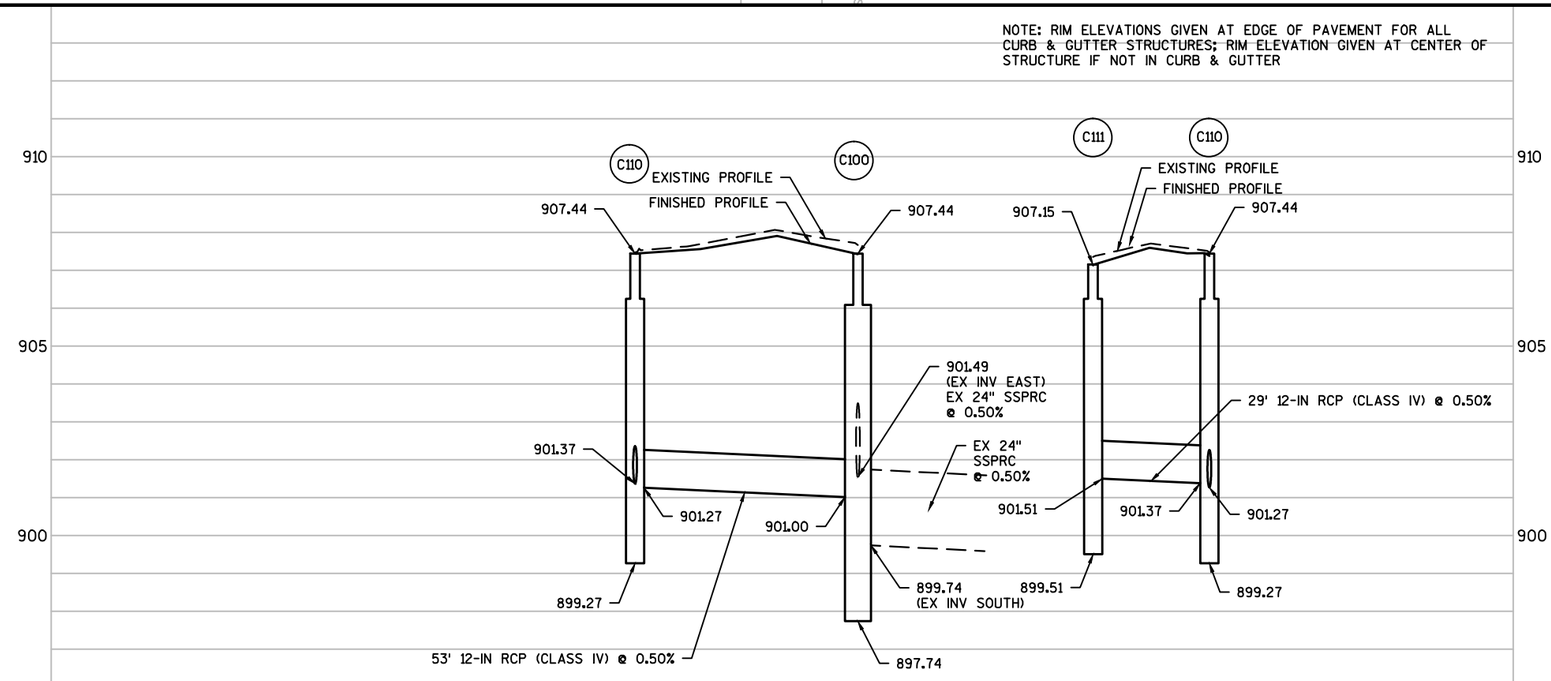
NOTE: RIM ELEVATIONS GIVEN AT EDGE OF PAVEMENT FOR ALL CURB & GUTTER STRUCTURES; RIM ELEVATION GIVEN AT CENTER OF STRUCTURE IF NOT IN CURB & GUTTER



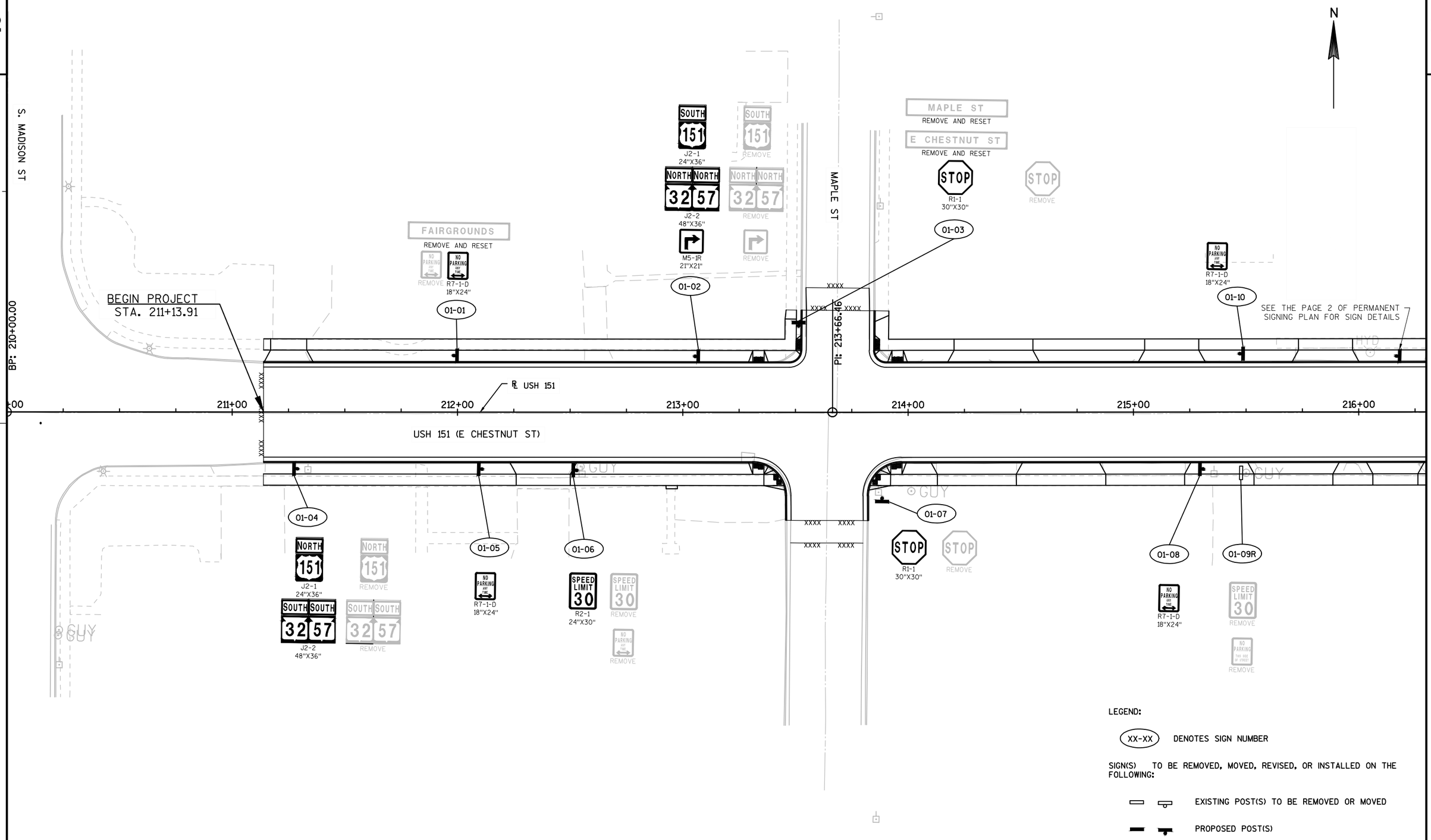
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	STORM SEWER: USH 151	SHEET 24 E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\022501_SS.DWG
LAYOUT NAME - 050102_SS
PLOT DATE : 4/22/2015 10:02 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:40_XREF
WISDOT/CADDS SHEET 44





PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	STORM SEWER: USH 151	SHEET 25	E



LEGEND:

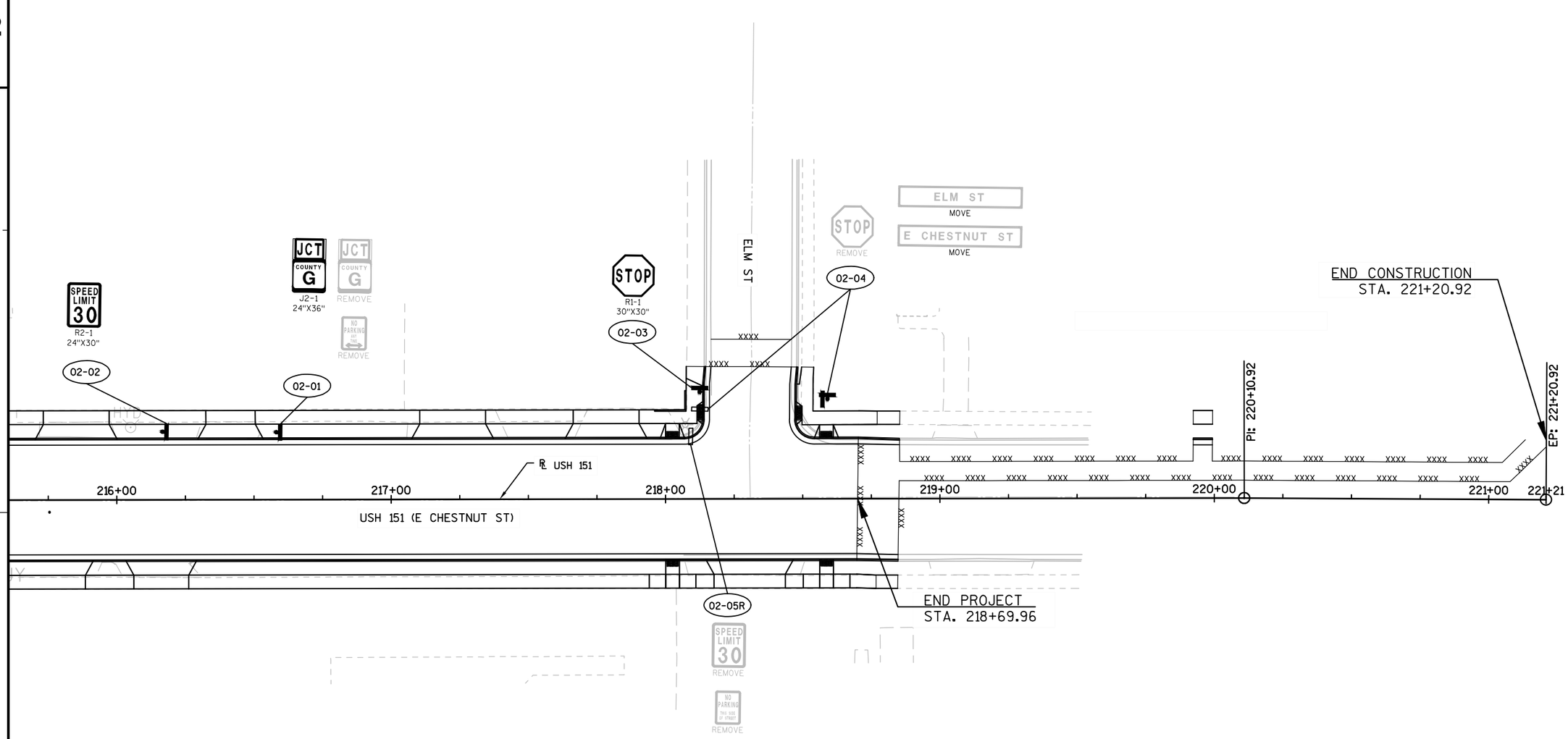
(XX-XX) DENOTES SIGN NUMBER

SIGN(S) TO BE REMOVED, MOVED, REVISED, OR INSTALLED ON THE FOLLOWING:

EXISTING POST(S) TO BE REMOVED OR MOVED

PROPOSED POST(S)

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PERMANENT SIGNING	SHEET 26	E



LEGEND:

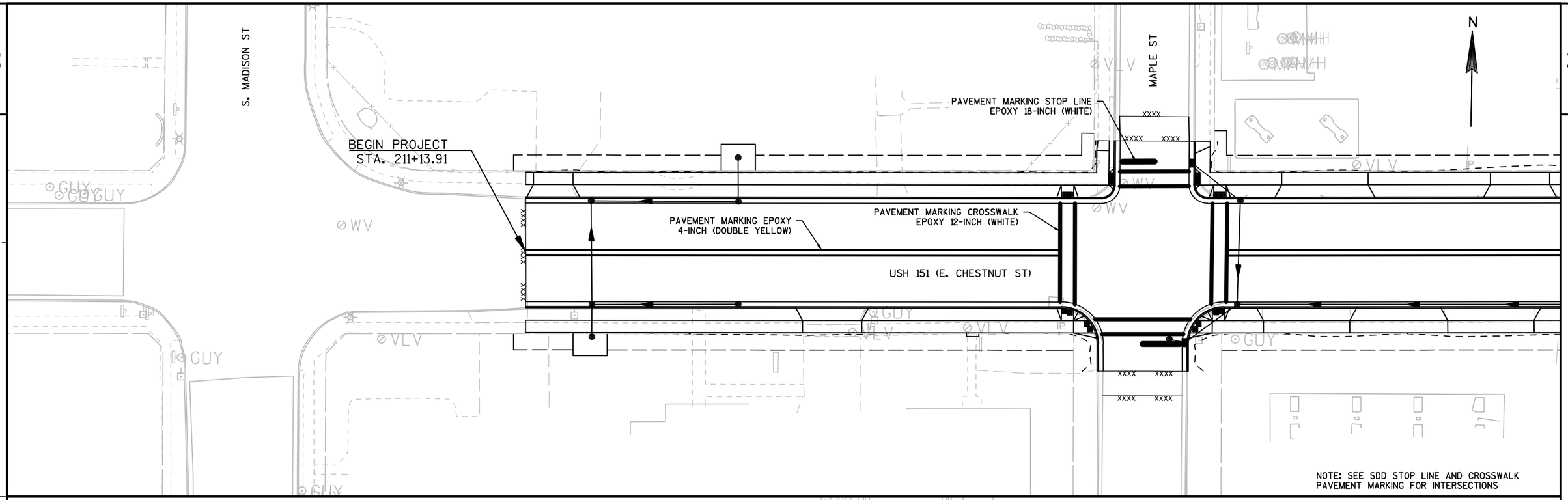
XX-XX DENOTES SIGN NUMBER

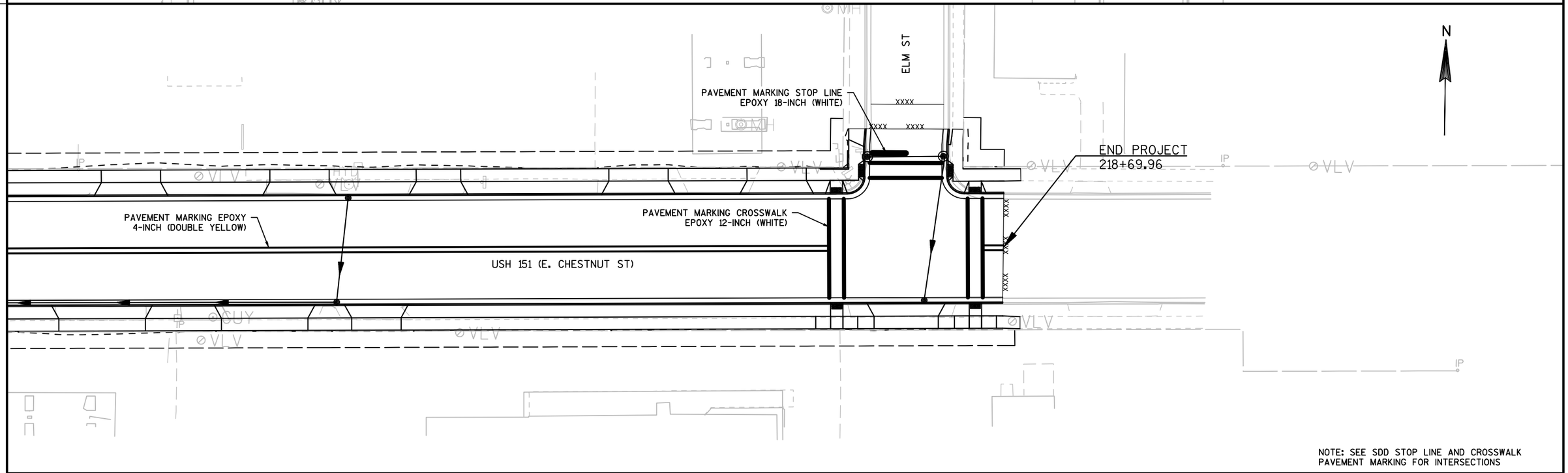
SIGN(S) TO BE REMOVED, MOVED, REVISED, OR INSTALLED ON THE FOLLOWING:

EXISTING POST(S) TO BE REMOVED OR MOVED

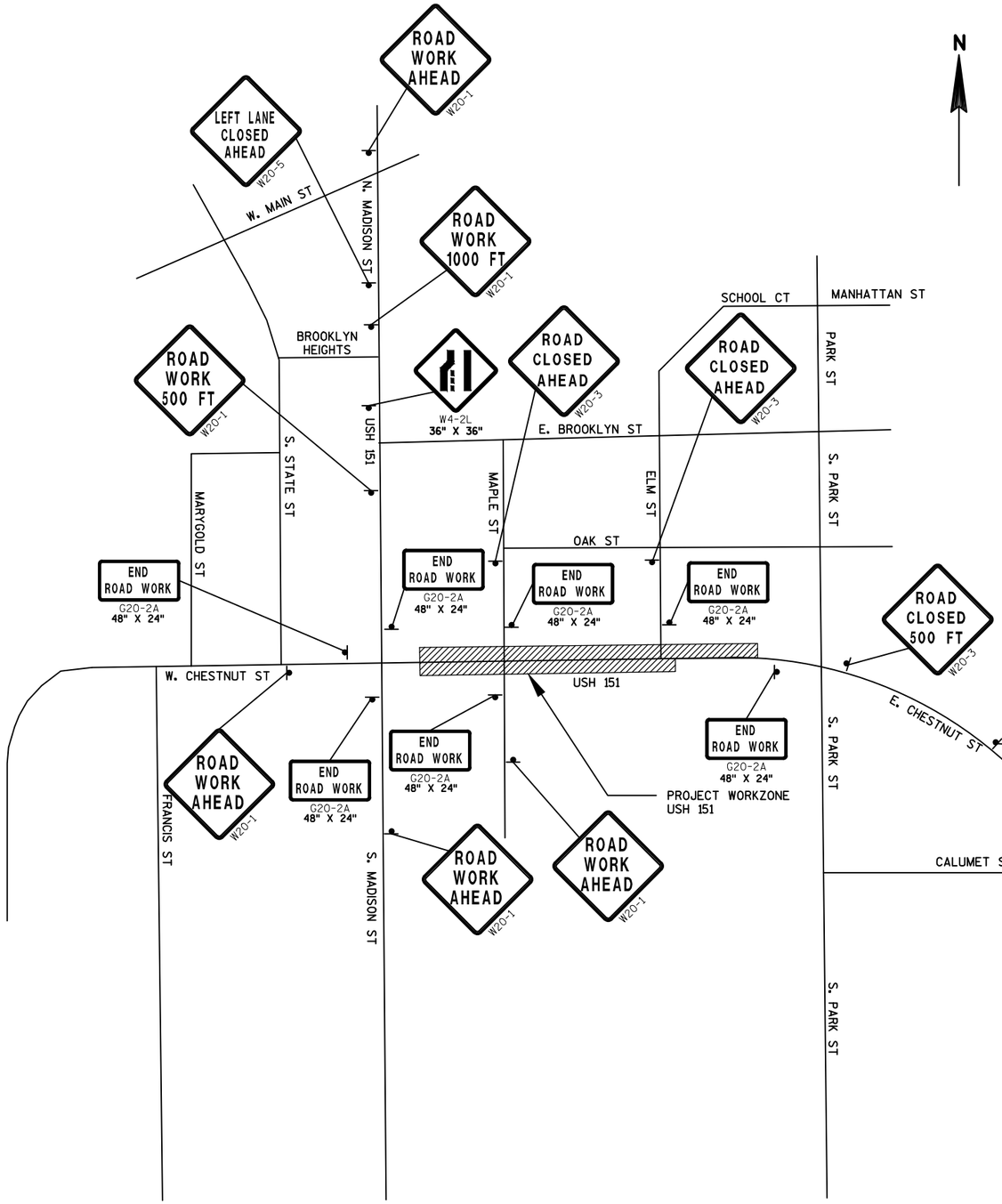
PROPOSED POST(S)

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PERMANENT SIGNING	SHEET 27	E





PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PAVEMENT MARKING	SHEET 28	E



STAGE TRAFFIC CONTROL NOTES:

CLOSE ROAD ON E. CHESTNUT STREET (USH 151/STH 32/57) TO TRAFFIC FROM S. MADISON STREET TO ELM STREET (EASTBOUND). CLOSE ROAD ON E. CHESTNUT STREET TO THRU TRAFFIC FROM ELM STREET TO S. MADISON STREET (WESTBOUND), HOWEVER MAINTAIN ONE WAY TRAFFIC IN THIS DIRECTION FOR LOCAL ACCESS. MAINTAIN DRIVEWAY ACCESS WITHIN THE WORK ZONE AREA. AT A MINIMUM MAINTAIN ROADWAY AND DRIVEWAY SURFACES WITH BASE AGGREGATE. MAINTAIN ACCESS FOR EMERGENCY VEHICLES AT ALL TIMES. CONSTRUCT PIPE CROSSINGS IN HALVES TO MAINTAIN ONE WAY WESTBOUND TRAFFIC AT ALL TIMES.

CLOSE THE LEFT TURN ONLY LANE ON S. MADISON STREET TO E. CHESTNUT STREET FOR THE DURATION OF THE PROJECT.

SETUP DETOUR PRIOR TO BEGINNING CONSTRUCTION AND MAINTAIN THROUGH THE COMPLETION OF THE PROJECT. CONFIRM ALL DETOUR SIGNING IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES LATEST EDITION. SUBMIT THE PROPOSED DETOUR AND SIGNING PLAN TO THE CITY OF CHILTON AND CALUMET COUNTY HIGHWAY DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO BEGINNING CONSTRUCTION OPERATIONS.

PROVIDE FLAGGERS AND SIGNING IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES LATEST EDITION WHEN TEMPORARY LANE SHIFTS ARE REQUIRED FOR PIPE OR UTILITY CROSSINGS. WHEN MORE THAN ONE PIPE OR UTILITY CROSSING IS OCCURRING AT THE SAME TIME, PROVIDE SEPARATE SIGNING AND FLAGGERS FOR EACH OPERATION UNLESS THEY ARE IN CLOSE PROXIMITY TO BE CONSOLIDATED INTO A SINGLE SIGNING AND FLAGGING OPERATION.

PHASE THE CONSTRUCTION ON MAPLE STREET TO THE SOUTH TO MAINTAIN LOCAL ACCESS AT ALL TIMES.

PORTABLE SUPPORTS ALLOWED FOR SIGN PLACEMENT ON PAVEMENT AND AGGREGATE SURFACES ONLY.

CONFIRM DETAILS OF TRAFFIC CONTROL DEVICES AND THEIR LOCATION NOT SHOWN ON THE TRAFFIC CONTROL DRAWINGS WITH THE PERTINENT REQUIREMENTS OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES LATEST EDITION. ADJUST SIGNS, BARRICADES, AND DRUM LOCATIONS AS DIRECTED BY THE ENGINEERING IN THE FIELD.
CONFORM ALL SIGN SIZES TO THE REQUIREMENTS FOR A CONVENTIONAL ROAD AS DESCRIBED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. THE MINIMUM SIZE FOR A DIAMOND SHAPED TEMPORARY TRAFFIC CONTROL SIGN IS 48" X 48".

REMOVE ALL PAVEMENT MARKING THAT IS NOT APPROPRIATE TO THE TRAVEL PATH.

PLACE ADDITIONAL DRUMS AT SIDE ROADS AND PRIVATE ENTRANCES AS DIRECTED BY THE ENGINEER IN THE FIELD.

EXISTING UTILITIES ARE NOT SHOWN ON TRAFFIC CONTROL SHEETS.

SEE SDD BARRICADES AND SIGNS FOR MAINLINE CLOSURES AND SDD DETOUR SIGNING FOR MAINLINE CLOSURES FOR ADVANCED SIGNING AND MARKING.

SEE SDD BARRICADES AND SIGNS FOR SIDEROAD CLOSURES FOR SIGNING AND MARKING OF MAPLE STREET AND ELM STREET CLOSURES.

SEE SDD TRAFFIC CONTROL, SIDEWALK CLOSURE FOR SIGNING AND MARKING FOR A CLOSED SIDEWALK DURING CONSTRUCTION. ONE SIDEWALK ON EITHER SIDE OF THE STREET MUST REMAIN OPEN AT ALL TIMES.

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	TRAFFIC CONTROL: ADDITIONAL SIGNING AND MARKING	SHEET 30	E

2

OAK ST

OAK ST

N

ELM ST

S. PARK ST

SEE SDD BARRICADES AND SIGNS FOR SIDEROAD CLOSURES (DETAIL 3)

SEE SDD TRAFFIC CONTROL SIDEWALK CLOSURE

SEE SDD TRAFFIC CONTROL SIDEWALK CLOSURE

℄ USH 151

PI: 220+10.92

EP: 221+20.92

216+00

217+00

218+00

219+00

220+00

221+00

221+21

25'

E. CHESTNUT ST (USH 151)

R11-2
48" X 30"
LANE CLOSED
TYPE III BARRICADE

SEE SDD TRAFFIC CONTROL SIDEWALK CLOSURE

ROAD CLOSED TO THRU TRAFFIC
R11-4
60" X 30"
TYPE III BARRICADE

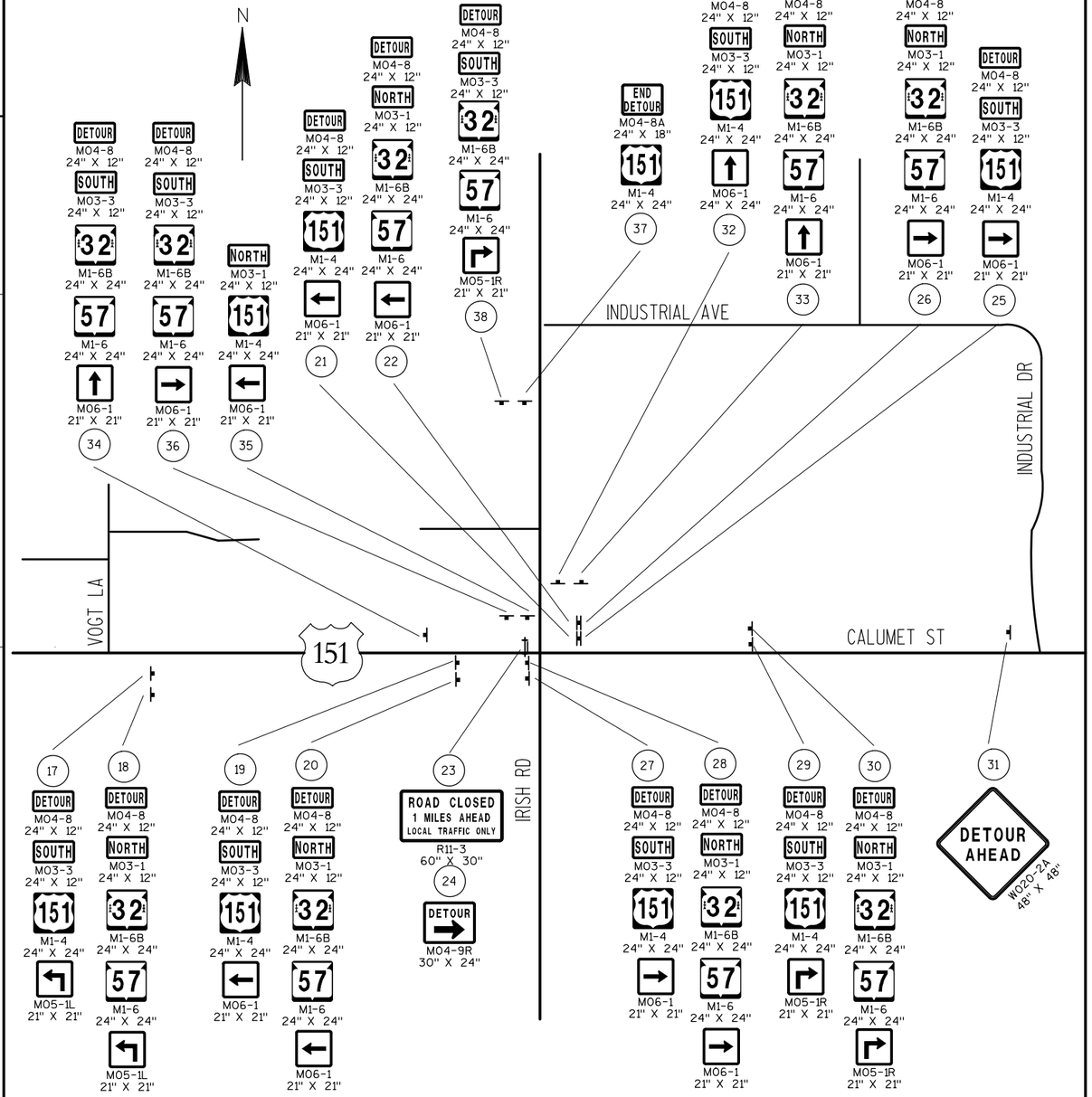
ROAD CLOSED TO THRU TRAFFIC
R11-4
60" X 30"
TYPE III BARRICADE

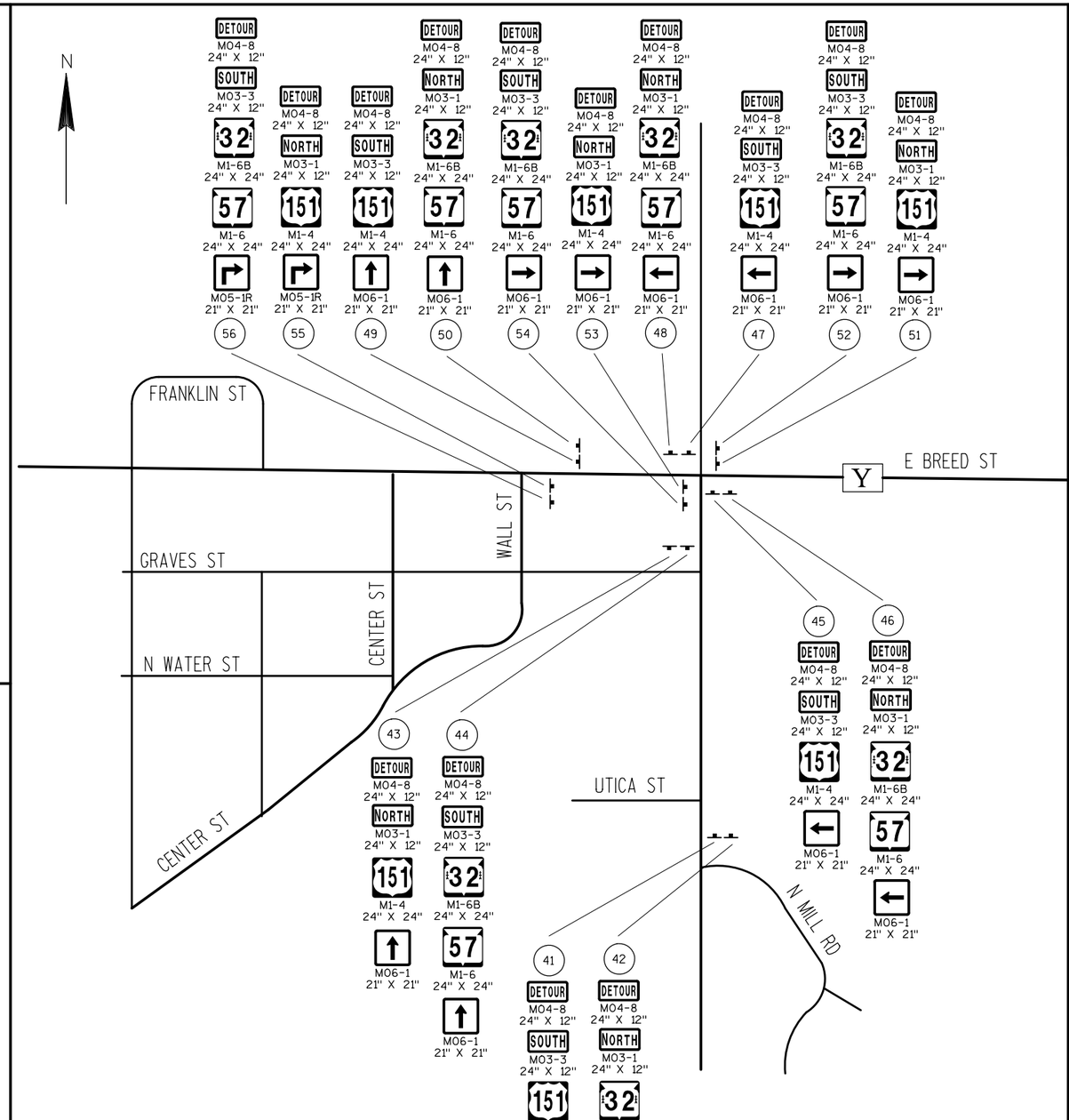
SEE SDD TRAFFIC CONTROL SIDEWALK CLOSURE

SEE SDD BARRICADES AND SIGNS FOR MAINLINE CLOSURES

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	TRAFFIC CONTROL: ADDITIONAL SIGNING AND MARKING	SHEET 31	E

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	DETOUR SIGNING DETAIL	SHEET 32	E





LEGEND

- (X) SIGN NUMBER. REFER TO MISCELLANEOUS QUANTITY SHEET
- ▲ PCMS (X) PORTABLE CHANGEABLE MESSAGE SIGN
- SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

DETAIL 1

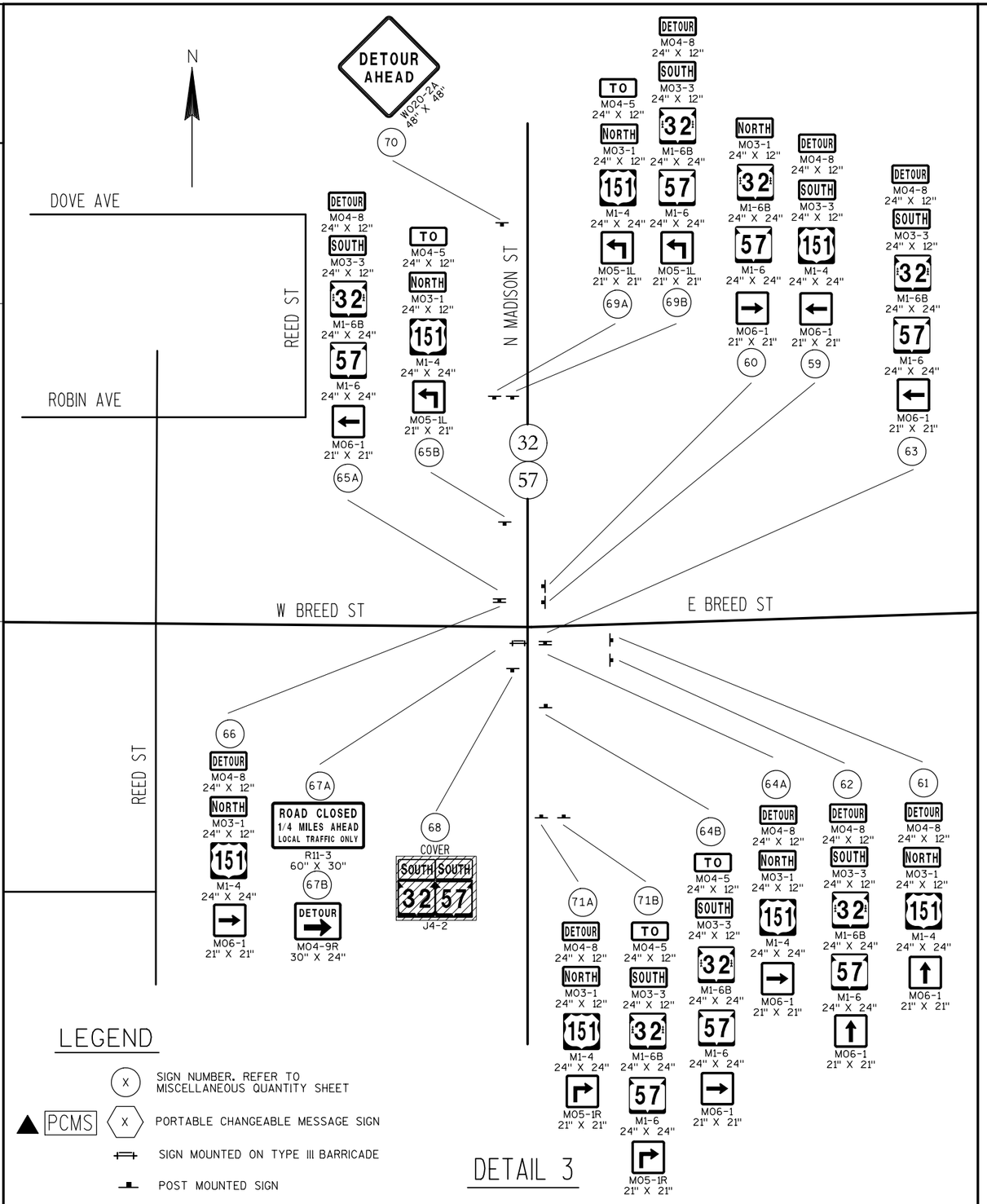
DETAIL 2

SHEET 2 OF 3

PLAN SHEET PRODUCED BY WisDOT-NE REGION

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	DETOUR SIGNING DETAIL	SHEET 33	E

FILE NAME : Y:\Milwaukee\20100S\20140.00\41003100\SheetsPlan\027002_dt.dgn
PLOT DATE : 3/19/2014
PLOT BY : twatt
PLOT NAME :
PLOT SCALE : 1:200
WISDOT/CADDS SHEET 42

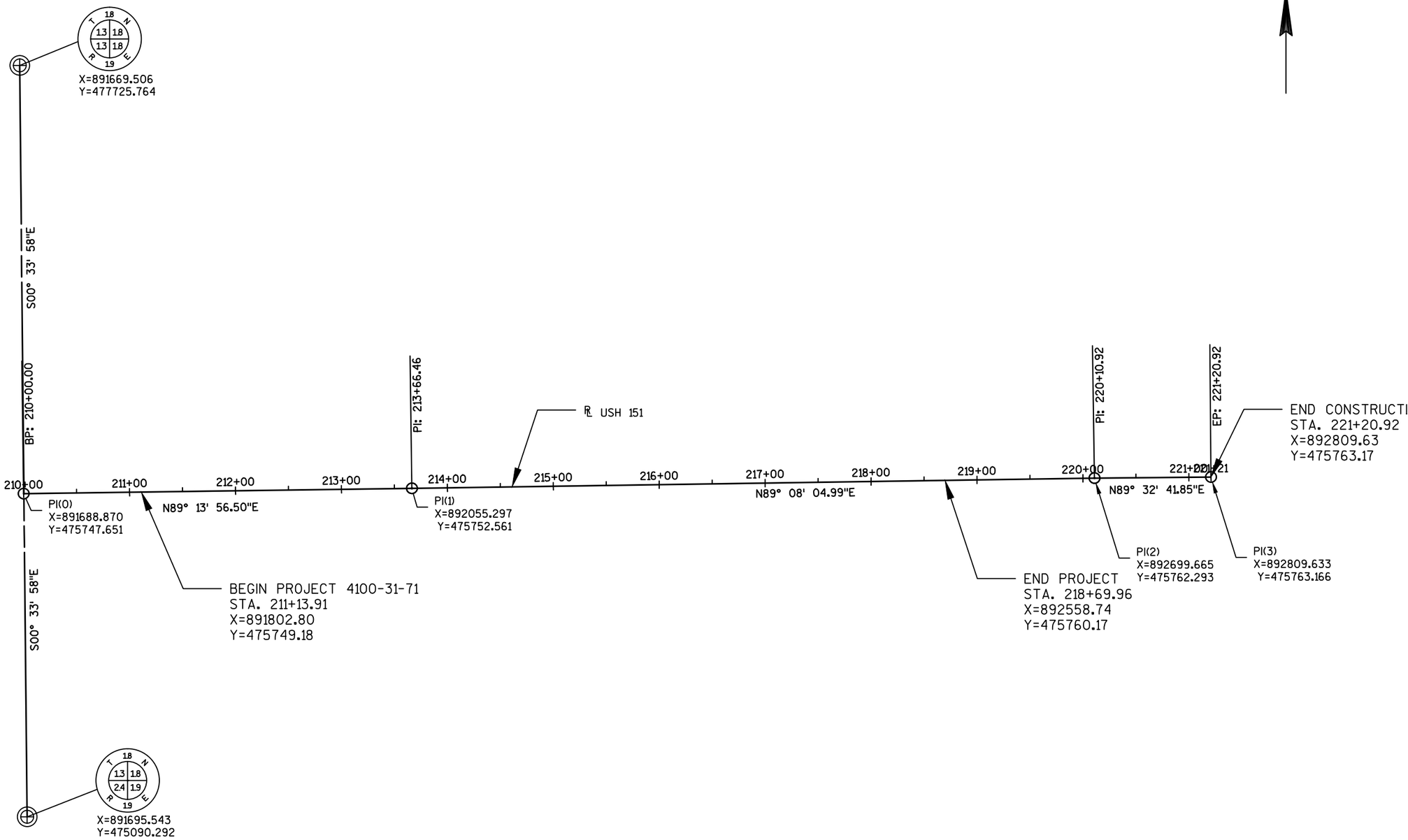


SHEET 3 OF 3

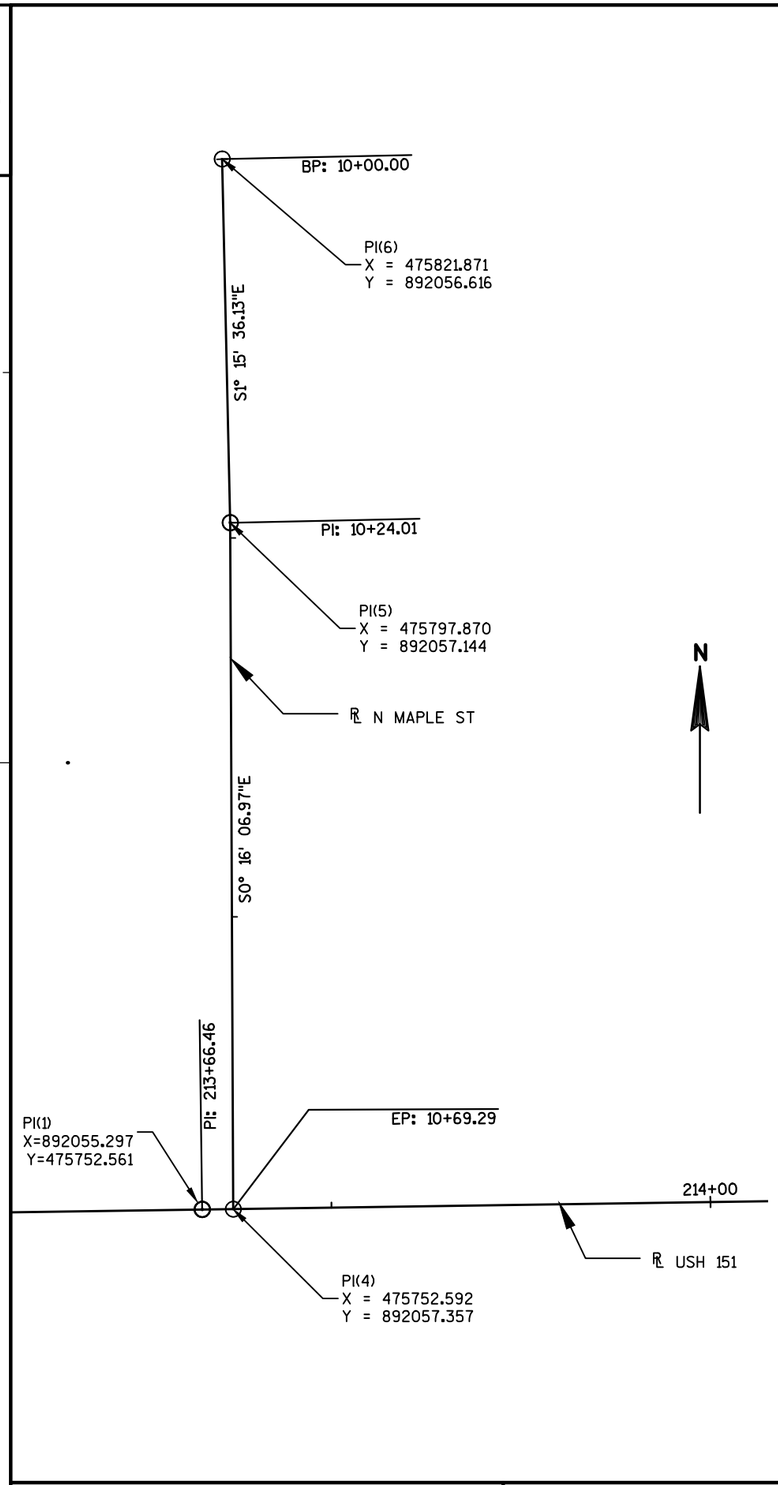
PLAN SHEET PRODUCED BY WisDOT-NE REGION

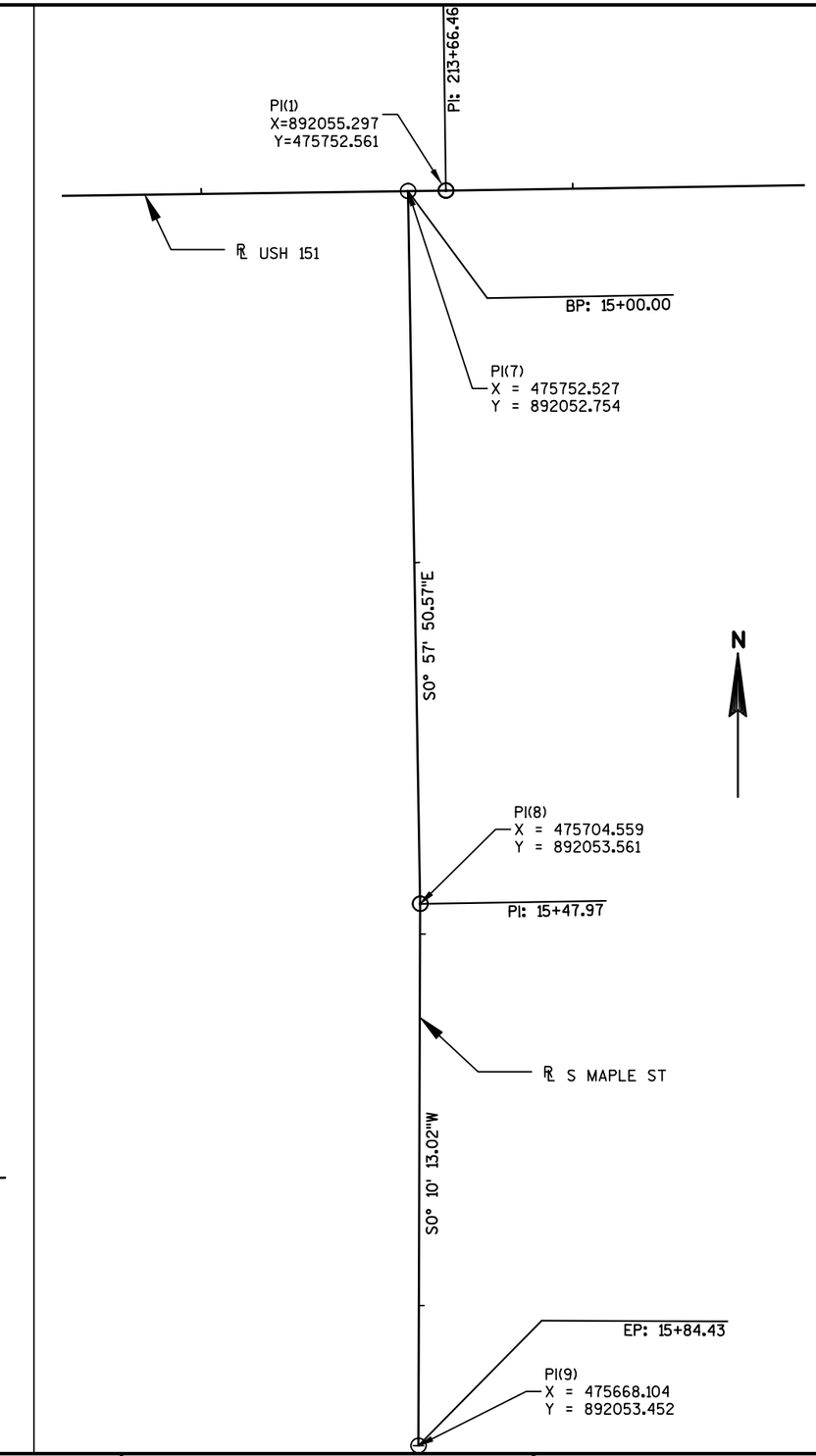
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	DETOUR SIGNING DETAIL	SHEET 34	E

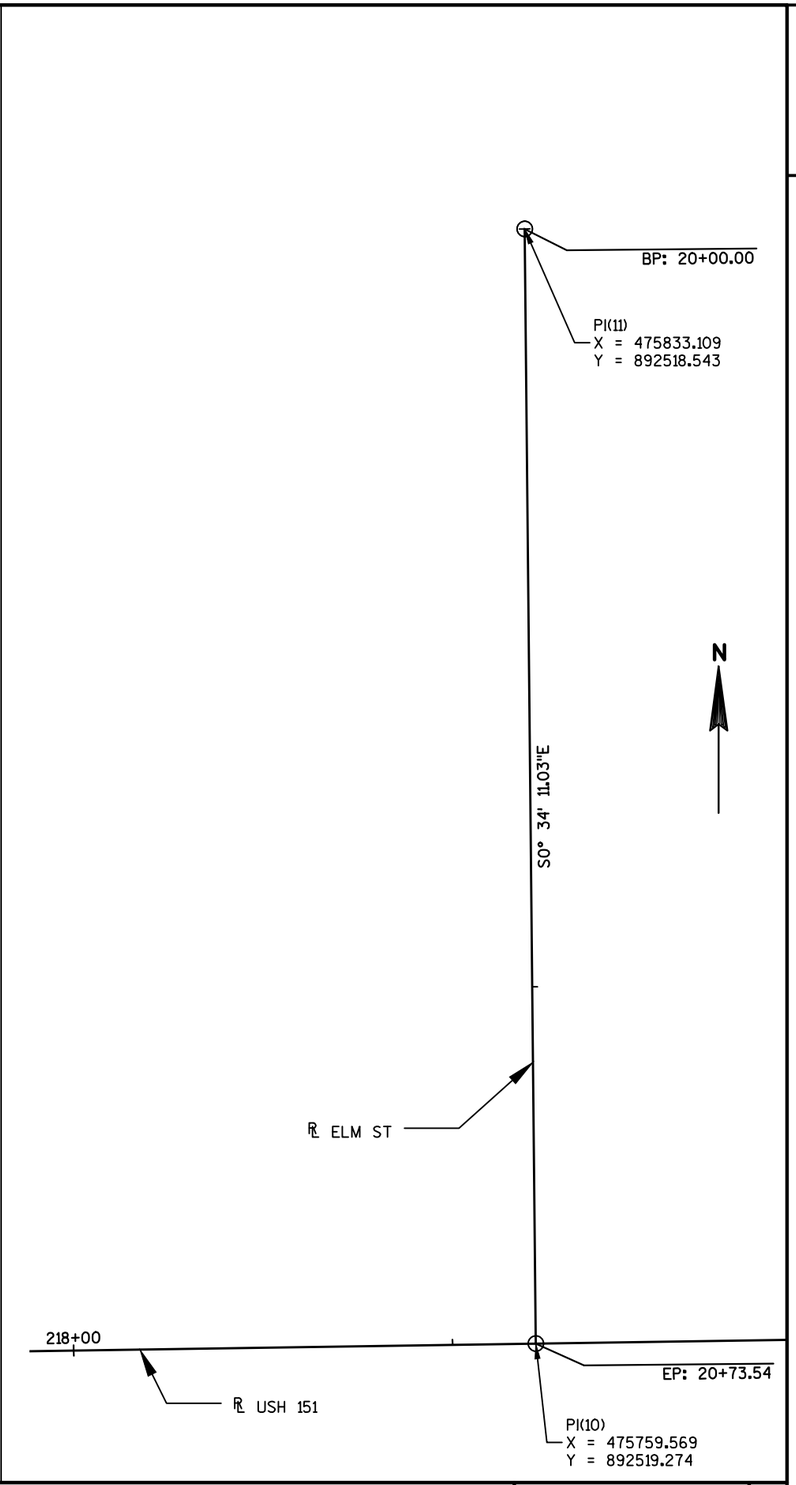
2



PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	ALIGNMENT PLAN - USH 151	SHEET 35	E







PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	ALIGNMENT PLAN - SIDE ROADS	SHEET	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\027202_AD.DWG PLOT DATE : 4/22/2015 10:04 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

DATE 09JUN15 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	4100-31-71 QUANTITY
0010	203.0210.S	Abatement of Asbestos Containing Material (structure) 01. Water Main	LS	1.000	1.000
0020	204.0100	Removing Pavement	SY	3,800.000	3,800.000
0030	204.0110	Removing Asphaltic Surface	SY	130.000	130.000
0040	204.0150	Removing Curb & Gutter	LF	1,665.000	1,665.000
0050	204.0155	Removing Concrete Sidewalk	SY	870.000	870.000
0060	204.0210	Removing Manholes	EACH	5.000	5.000
0070	204.0220	Removing Inlets	EACH	8.000	8.000
0080	204.0245	Removing Storm Sewer (size) 01. 8-Inch	LF	35.000	35.000
0090	204.0245	Removing Storm Sewer (size) 02. 12-Inch	LF	770.000	770.000
0100	204.0291.S	Abandoning Sewer	CY	10.000	10.000
0110	205.0100	Excavation Common	CY	4,077.980	4,077.980
0120	213.0100	Finishing Roadway (project) 01. 4100-31-71	EACH	1.000	1.000
0130	305.0110	Base Aggregate Dense 3/4-Inch	TON	1,520.000	1,520.000
0140	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2,080.000	2,080.000
0150	311.0110	Breaker Run	TON	4,270.000	4,270.000
0160	415.0080	Concrete Pavement 8-Inch	SY	3,730.000	3,730.000
0170	415.0210	Concrete Pavement Gaps	EACH	3.000	3.000
0180	416.0160	Concrete Driveway 6-Inch	SY	210.000	210.000
0190	416.0610	Drilled Tie Bars	EACH	50.000	50.000
0200	416.0620	Drilled Dowel Bars	EACH	25.000	25.000
0210	455.0120	Asphaltic Material PG64-28	TON	12.000	12.000
0220	455.0605	Tack Coat	GAL	40.000	40.000
0230	460.1103	HMA Pavement Type E-3	TON	155.000	155.000
0240	460.2000	Incentive Density HMA Pavement	DOL	110.000	110.000
0250	520.8000	Concrete Collars for Pipe	EACH	2.000	2.000
0260	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	1,650.000	1,650.000
0270	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	55.000	55.000
0280	601.0600	Concrete Curb Pedestrian	LF	20.000	20.000
0290	602.0410	Concrete Sidewalk 5-Inch	SF	6,175.000	6,175.000
0300	602.0415	Concrete Sidewalk 6-Inch	SF	1,700.000	1,700.000
0310	602.0420	Concrete Sidewalk 7-Inch	SF	830.000	830.000
0320	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	112.000	112.000
0330	608.0312	Storm Sewer Pipe Reinforced Concrete Class III 12-Inch	LF	145.000	145.000
0340	608.0412	Storm Sewer Pipe Reinforced Concrete Class IV 12-Inch	LF	467.000	467.000
0350	611.0612	Inlet Covers Type C	EACH	1.000	1.000
0360	611.0651	Inlet Covers Type S	EACH	2.000	2.000
0370	611.0660	Inlet Covers Type WM	EACH	13.000	13.000
0380	611.1004	Catch Basins 4-FT Diameter	EACH	14.000	14.000
0390	611.1006	Catch Basins 6-FT Diameter	EACH	2.000	2.000
0400	618.0100	Maintenance And Repair of Haul Roads (project) 01. 4100-31-71	EACH	1.000	1.000
0410	619.1000	Mobilization	EACH	1.000	1.000
0420	624.0100	Water	MGAL	16.000	16.000
0430	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0440	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0450	628.7005	Inlet Protection Type A	EACH	3.000	3.000
0460	628.7015	Inlet Protection Type C	EACH	23.000	23.000
0470	629.0205	Fertilizer Type A	CWT	57.000	57.000
0480	630.0140	Seeding Mixture No. 40	LB	4.000	4.000
0490	631.0300	Sod Water	MGAL	15.000	15.000

3

DATE 09JUN15 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	4100-31-71 QUANTITY
0500	631.1000	Sod Lawn	SY	583.000	583.000
0510	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	9.000	9.000
0520	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	2.000	2.000
0530	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	1.000	1.000
0540	634.0620	Posts Wood 4x6-Inch X 20-FT	EACH	1.000	1.000
0550	637.2210	Signs Type II Reflective H	SF	85.610	85.610
0560	638.2102	Moving Signs Type II	EACH	5.000	5.000
0570	638.2602	Removing Signs Type II	EACH	10.000	10.000
0580	638.3000	Removing Small Sign Supports	EACH	8.000	8.000
0590	642.5001	Field Office Type B	EACH	1.000	1.000
0600	643.0100	Traffic Control (project) 01. 4100-31-71	EACH	1.000	1.000
0610	643.0300	Traffic Control Drums	DAY	3,974.000	3,974.000
0620	643.0410	Traffic Control Barricades Type II	DAY	3,070.000	3,070.000
0630	643.0420	Traffic Control Barricades Type III	DAY	3,058.000	3,058.000
0640	643.0705	Traffic Control Warning Lights Type A	DAY	8,236.000	8,236.000
0650	643.0715	Traffic Control Warning Lights Type C	DAY	1,218.000	1,218.000
0660	643.0900	Traffic Control Signs	DAY	4,610.000	4,610.000
0670	643.0920	Traffic Control Covering Signs Type II	EACH	12.000	12.000
0680	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0690	643.2000	Traffic Control Detour (project) 01. 4100-31-71	EACH	1.000	1.000
0700	643.3000	Traffic Control Detour Signs	DAY	35,746.000	35,746.000
0710	646.0106	Pavement Marking Epoxy 4-Inch	LF	1,270.000	1,270.000
0720	647.0566	Pavement Marking Stop Line Epoxy 18-Inch	LF	50.000	50.000
0730	647.0776	Pavement Marking Crosswalk Epoxy 12-Inch	LF	520.000	520.000
0740	650.4000	Construction Staking Storm Sewer	EACH	14.000	14.000
0750	650.4500	Construction Staking Subgrade	LF	831.000	831.000
0760	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	65.000	65.000
0770	650.7000	Construction Staking Concrete Pavement	LF	831.000	831.000
0780	650.9910	Construction Staking Supplemental Control (project) 01. 4100-31-71	LS	1.000	1.000
0790	690.0150	Sawing Asphalt	LF	1,485.000	1,485.000
0800	690.0250	Sawing Concrete	LF	305.000	305.000
0810	715.0415	Incentive Strength Concrete Pavement	DOL	500.000	500.000
0820	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	300.000	300.000
0830	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	600.000	600.000
0840	SPV.0060	Special 01. Adjusting Water Valve Boxes	EACH	1.000	1.000
0850	SPV.0060	Special 02. Abandon Water Valve	EACH	1.000	1.000
0860	SPV.0060	Special 03. Remove Fire Hydrant	EACH	1.000	1.000
0870	SPV.0060	Special 04. Cut and Plug Existing Water Main 6-Inch	EACH	2.000	2.000
0880	SPV.0060	Special 05. Tee 8" X 6"	EACH	1.000	1.000
0890	SPV.0060	Special 06. Bend 45-Degree 8"	EACH	10.000	10.000
0900	SPV.0060	Special 07. Bend 45-Degree 6" With 8" X 6" Reducer	EACH	2.000	2.000
0910	SPV.0060	Special 08. Remove Water Service Curb Stop	EACH	11.000	11.000
0920	SPV.0060	Special 09. Water Service Curb Stop	EACH	10.000	10.000
0930	SPV.0060	Special 10. Connect to Existing Water Main	EACH	3.000	3.000
0940	SPV.0060	Special 11. Water Valve and Box 6-Inch	EACH	1.000	1.000
0950	SPV.0060	Special 12. Water Valve and Box 8-Inch	EACH	1.000	1.000

3

DATE 09JUN15 E S T I M A T E O F Q U A N T I T I E S

LINE NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	4100-31-71 QUANTITY
0960	SPV.0060	Special 13. Adjusting Sanitary Manhole Cover	EACH	2.000	2.000
0970	SPV.0090	Special 01. Water Main 6-Inch PVC	LF	10.000	10.000
0980	SPV.0090	Special 02. Water Main 8-Inch	LF	1,030.000	1,030.000
0990	SPV.0090	Special 03. Water Service 1-Iinch	LF	325.000	325.000
1000	SPV.0090	Special 04. Removing Asbestos Water Main	LF	25.000	25.000
1010	SPV.0105	Special 01. Concrete Pavement Joint Layout	LS	1.000	1.000
1020	SPV.0105	Special 02. Construction Staking Water Main	LS	1.000	1.000
1030	SPV.0105	Special 03. Temporary Water	LS	1.000	1.000

3

REMOVALS

LOCATION	CATEGORY	ITEM 204.0100 REMOVING PAVEMENT (SY)	ITEM 204.0110 REMOVING ASPHALTIC SURFACE (SY)	ITEM 204.0150 REMOVING CURB & GUTTER (LF)	ITEM 204.0155 REMOVING CONCRETE SIDEWALK (SY)	ITEM 204.0210 REMOVING MANHOLES (EACH)	ITEM 204.0220 REMOVING INLETS (EACH)	ITEM 204.0245.01 REMOVING STORM SEWER 8-INCH (LF)	ITEM 204.0245.02 REMOVING STORM SEWER 12-INCH (LF)	ITEM 204.0291.S ABANDONING SEWER (CY)
211+13 to 218+69	0010	3650	118	1571	815	5	7	31	734	7
218+69 to 218+69	0010	-	-	45	12	-	-	-	-	-
UNDISTRIBUTED	0010	150	12	49	43	-	1	4	36	3
TOTALS		**3800**	**130**	**1665**	**870**	**5**	**8**	**35**	**770**	**10**

PAVEMENT

LOCATION	CATEGORY	ITEM 305.0110 BASE AGGREGATE DENSE 3/4-INCH (TON)	ITEM 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH (TON)	ITEM 311.0110 BREAKER RUN (TON)	ITEM 415.0080 CONCRETE PAVEMENT 8-INCH (SY)	ITEM 415.0210 CONCRETE PAVEMENT GAPS (EA)	ITEM 416.0160 CONCRETE DRIVEWAY 6-INCH (SY)	ITEM 416.0610 DRILLED TIE BARS (EACH)	ITEM 416.0620 DRILLED DOWEL BARS (EACH)	ITEM 601.0409 CONCRETE CURB & GUTTER 30-INCH TYPE A (LF)	ITEM 601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D (LF)	ITEM 601.0600 CONCRETE CURB PEDESTRIAN (LF)	ITEM 602.0410 CONCRETE SIDEWALK 5-INCH (SF)	ITEM 602.0415 CONCRETE SIDEWALK 6-INCH (SF)	ITEM 602.0420 CONCRETE SIDEWALK 7-INCH (SF)	ITEM 602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW (SF)	ITEM SPV.0105.01 CONCRETE PAVEMENT JOINT LAYOUT (LS)
211+13 to 218+69	0010	885	1372	3659	3361	3	-	38	8	1415	-	-	5375	1700	-	16	1
MAPLE ST NORTH	0010	-	31	108	82	-	-	-	4	52	-	-	260	-	340	32	-
MAPLE ST SOUTH	0010	-	33	128	114	-	-	-	4	52	-	-	-	-	205	32	-
ELM ST	0010	-	30	95	96	-	-	-	4	52	-	19	150	-	273	32	-
DRIVEWAYS	0010	-	55	-	-	-	190	-	-	-	-	-	-	-	-	-	-
PARKING LOTS	0010	-	10	-	20	-	-	-	-	-	-	-	-	-	-	-	-
SIDEWALKS	0010	-	55	-	-	-	-	-	-	-	-	-	-	-	-	-	-
218+69 to 221+21	0010	550	180	-	-	-	-	-	-	-	55	-	200	-	-	-	-
UNDISTRIBUTED	0010	85	314	280	57	-	20	12	5	79	-	1	190	-	12	-	-
TOTALS		**1520**	**2080**	**4270**	**3730**	**3**	**210**	**50**	**25**	**1650**	**55**	**20**	**6175**	**1700**	**830**	**112**	**1**

ASPHALT

LOCATION	CATEGORY	ITEM 455.0120 ASPHALTIC MATERIAL PG64-28 (TON)	ITEM 455.0605 TACK COAT (GAL)	ITEM 460.1103 HMA PAVEMENT TYPE E-3 (TON)
218+69 to 221+21	0010	6	26	92
S Maple Street	0010	1	2	9
N Maple Street	0010	1	2	9
Elm Street	0010	1	2	9
DRIVEWAYS & PARKING LOTS	0010	2	3	26
UNDISTRIBUTED	0010	1	5	10
TOTALS		**12**	**40**	**155**

3

EARTHWORK SUMMARY

Division	From / To Station	Location	Common Excavation (1)	(item # 205.0100)	Unusable Pavement Material (4)	Available Material (5)	Unexpanded Fill	Expanded Fill (6)	Mass Ordinate + / - (7)	Waste	Borrow	Comment:
			Cut (2)	EBS Excavation (3)				Factor 1.33			(item #208.0100)	
Division 1 (Category 0010)												
S. Maple Street	15+20.01/15+47.91	S. Maple St	87	0	38	48	0	0	48	48	0	
N. Maple Street	15+20.01/10+49.25	N. Maple St	82	0	34	48	0	0	48	48	0	
Elm Street	10+24.03/20+53.54	Elm St	80	0	40	40	0	0	40	40	0	
USH 151 (E. Chestnut Street)	211+13.91/218+69.69	USH 151	3,729	0	1,353	2,376	14	19	2,358	2,358	0	
Division 1 Subtotal			3,978	0	1,465	2,513	14	19	2,494	2,494	0	
Division 2 (Category 0010)												
Undistributed			0	100	0	100	0	0	100	100	0	
Division 2 Subtotal			0	100	0	100	0	0	100	100	0	
Grand Total			3,977.98	100.00	1,465.30	2,612.68	14.21	18.90	2,593.78	2,593.78	0.00	
Total Common Exc				4,077.98								

1) Common Excavation is the sum of the Cut and EBS Excavation columns. Item number 205.0100
2) Unsuable Pavement Material is included in Cut.
3) EBS Excavation to be considered Waste
4) Unusable Pavement Material is paid for under Items Removing Pavement, Removing Concrete Sidewalk, and Removing Curb & Gutter
5) Available Material = Cut - Unusuable Pavement Material
6) Expanded Fill. Factor = 1.33
Depending on selections: **Expanded Fill = Unexpanded Fill * Fill Factor**
7) The Mass Ordinate + or - Qty calculated for the Division. Plus quantity indicates an excess of material within the Division. Minus indicates a shortage of material within the Division.
Note: EBS Excavation Areas - This is to be filled with Breaker Run material and is paid for under Item Breaker Run

3

STORM SEWER PIPES

PIPE NO. & UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	LENGTH	SLOPE	CATEGORY	ITEM 520.8000 CONCRETE COLLARS FOR PIPE (EACH)	ITEM 608.0312 STORM SEWER PIPE REINFORCED CONCRETE CLASS III 12-INCH (LF)	ITEM 608.0412 STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 12-INCH (LF)
A105	A100	55	0.50%	0010	-	55	-
A152	A150	56	0.50%	0010	-	56	-
B105	B101	194	0.50%	0010	-	-	194
B101	B100	7	0.50%	0010	-	-	7
B102	B101	27	0.50%	0010	-	-	27
A151	A150	10	0.50%	0010	-	10	-
A150	A100	40	0.50%	0010	-	-	40
A105	A106	18	0.27%	0010	-	18	-
B103	B102	40	0.50%	0010	-	-	40
B104	B103	25	0.50%	0010	-	-	25
B106	B105	41	0.50%	0010	-	-	41
C110	C100	53	0.50%	0010	-	-	53
C111	C110	29	0.50%	0010	-	-	29
	UNDISTRIBUTED			0010	2	6	11
			TOTALS		**2**	**145**	**467**

STORM SEWER STRUCTURES

STR	CATEGORY	ALIGNMENT RL	STATION	OFFSET		RIM OR GRATE ELEV	INVERT ELEVATION NORTH	NORTHEAST	SOUTHEAST	EAST	SOUTH	NORTHWEST	SOUTHWEST	WEST	611.0612 INLET COVERS TYPE C (EACH)	611.0651 INLET COVERS TYPE S (EACH)	ITEM 611.0660 INLET COVERS TYPE WM (EACH)	ITEM 611.1004 CATCH BASINS 4-FT DIAMETER (EACH)	ITEM 611.1006 CATCH BASINS 6-FT DIAMETER (EACH)	SPV.0060.13 ADJUSTING SANITARY MANHOLE COVER (EACH)
A100	0010	E CHESTNUT ST	211+40	20.00	LT	900.57	-	-	-	896.90	896.90	-	-	896.74	-	-	1	-	1	-
A105	0010	E CHESTNUT ST	212+00	20.00	LT	900.38	897.30	-	-	-	-	-	-	897.20	-	-	1	1	-	-
A106	0010	E CHESTNUT ST	212+00	38.50	LT	900.65	-	-	-	-	897.35	-	-	-	-	1	-	1	-	-
A150	0010	E CHESTNUT ST	211+40	20.00	RT	900.59	897.10	-	-	897.20	897.20	-	-	-	-	-	1	1	-	-
A151	0010	E CHESTNUT ST	211+40	34.50	RT	900.56	897.25	-	-	-	-	-	-	-	-	1	-	1	-	-
A152	0010	E CHESTNUT ST	212+00	20.00	RT	900.38	-	-	-	-	-	-	-	897.50	-	-	1	1	-	-
B100	0010	E CHESTNUT ST	213+74.85	34.91	RT	901.08	-	-	-	897.22	897.14	-	-	897.19 (ABANDON)	1	-	-	1	-	-
B101	0000	S. MAPLE ST	15+37	16.00	LT	901.01	-	897.30	-	-	-	-	-	897.25	-	-	1	1	-	-
B102	0010	E CHESTNUT ST	214+02	20.00	RT	901.21	897.50	-	-	897.50	-	-	897.44	-	-	-	1	1	-	-
B103	0010	E CHESTNUT ST	214+02	20.00	LT	901.21	-	-	-	-	897.70	897.75	-	-	-	-	1	1	-	-
B104	0010	N. MAPLE ST	10+33	14.00	LT	901.17	-	-	897.88	-	-	-	-	-	-	-	1	1	-	-
B105	0010	E CHESTNUT ST	216+00	20.00	RT	902.94	898.57	-	-	-	-	-	-	898.47	-	-	1	1	-	-
B106	0010	E CHESTNUT ST	216+05	20.00	LT	903.01	-	-	-	-	898.78	-	-	-	-	-	1	1	-	-
C100	0010	E CHESTNUT ST	218+37.96	20.00	RT	907.44	901.00	-	-	901.49	899.74	-	-	-	-	-	1	-	1	-
C110	0010	ELM ST	20+36.53	14.63	LT	907.44	-	-	-	-	901.27	-	-	901.37	-	-	1	1	-	-
C111	0010	ELM ST	20+36.53	14.63	RT	907.15	-	-	-	901.51	-	-	-	-	-	-	1	1	-	-
SANITARY MANHOLES	0020	E CHESTNUT ST	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2
														TOTALS	**1**	**2**	**13**	**14**	**2**	**2**

NOTE: RIM AND GRATE ELEVATIONS ARE TO FLANGE LINE OF CURB AND GUTTER FOR INLETS IN CURB AND GUTTER, ALL OTHER ELEVATION ARE TO CENTER OF STRUCTION

3

PERMANENT SIGNING, TYPE II

SIGN NO.	CATEGORY	SIGN CODE	SIGN TYPE	SIGN MESSAGE	TYPE II SIGN SIZE W [IN.]	H [IN.]	ITEM 634.0614 POSTS WOOD 4X6-INCH X 14-FT [EA]	ITEM 634.0616 POSTS WOOD 4X6-INCH X 16-FT [EA]	ITEM 634.0618 POSTS WOOD 4X6-INCH X 18-FT [EA]	ITEM 634.0620 POSTS WOOD 4X6-INCH X 20-FT [EA]	ITEM 637.2210 SIGNS TYPE II REFLECTIVE H [SF]	ITEM 638.2102 MOVING SIGNS TYPE II [EA]	ITEM 638.2602 REMOVING SIGNS TYPE II [EA]	ITEM 638.3000 REMOVING SMALL SIGN SUPPORTS [EA]	REMARKS
01-01(R)	0010	SPECIAL	II	FAIRGROUNDS	48	12	-	1	-	-	-	1	-	1	REMOVE AND RESET
	0010	R7-1-D	II	-	18	24	-	-	-	-	3.00	-	1	-	REMOVE AND REPLACE
01-02(R)	0010	J2-1	II	SOUTH	24	36	-	-	-	1	6.00	-	1	1	-
			II	[151]											
	0010	J2-2	II	NORTH NORTH	48	36	-	-	-	-	12.00	-	-	-	-
			II	[32] [57]											
	0010	M5-1R	II	[A]	21	21	-	-	-	-	3.06	-	-	-	-
01-03(R)	0010	R1-1	II	-	30	30	1	-	-	-	5.18	-	1	1	REMOVE AND REPLACE
	0010	SPECIAL	II	MAPLE ST	-	-	-	-	-	-	-	1	-	-	REMOVE AND RESET
	0010	SPECIAL	II	E CHESTNUT ST	-	-	-	-	-	-	-	1	-	-	REMOVE AND RESET
01-04(R)	0010	J2-1	II	NORTH	24	36	-	-	1	-	6.00	-	1	1	-
			II	[151]											
	0010	J2-2	II	SOUTH SOUTH	48	36	-	-	-	-	12.00	-	-	-	-
			II	[32] [57]											
01-05	0010	R7-1-D		-	18	24	1	-	-	-	3.00	-	-	-	-
01-06(R)	0010	R2-1	II	SPEED LIMIT 30	24	30	1	-	-	-	5.00	-	1	1	-
	0010	R7-1-D	II	-	18	24	-	-	-	-	3.00	-	-	-	-
01-07(R)	0010	R1-1	II	-	30	30	1	-	-	-	5.18	-	1	1	-
01-08	0010	R7-1-D	II	-	18	24	1	-	-	-	3.00	-	-	-	-
01-09R	0010	REMOVE	II	SPEED LIMIT 30	-	-	-	-	-	-	-	-	1	-	R2-1
	0010	REMOVE	II	-	-	-	-	-	-	-	-	-	-	-	R7-6-12
01-10	0010	R7-1-D	II	-	18	24	1	-	-	-	3.00	-	-	-	
02-01(R)	0010	J2-1	II	JCT	24	36	-	1	-	-	6.00	-	1	1	-
			II	[G]											
02-02	0010	R2-1	II	SPEED LIMIT 30	24	30	1	-	-	-	5.00	-	-	-	-
02-03	0010	R1-1		-	30	30	1	-	-	-	5.18	-	-	-	-
02-04	0010	R1-1	II	-	-	-	-	-	-	-	-	-	1	1	REMOVE
	0010	SPECIAL	II	ELM ST	-	-	1	-	-	-	-	1	-	-	MOVE
	0010	SPECIAL	II	E CHESTNUT ST	-	-	-	-	-	-	-	1	-	-	MOVE
02-05R	0010	REMOVE	II	SPEED LIMIT 30	-	-	-	-	-	-	-	-	1	-	R2-1
	0010	REMOVE	II	-	-	-	-	-	-	-	-	-	-	-	R7-6-12
(R) DENOTES REMOVAL OF EXISTING SIGN IN SAME LOCATION						**TOTALS**	**9**	**2**	**1**	**1**	**85.61**	**5**	**10**	**8**	

EROSION CONTROL

LOCATION		CATEGORY	ITEM 628.7005 INLET PROTECTION TYPE A (EA)	ITEM 628.7015 INLET PROTECTION TYPE C (EA)
Begin Project	RT	0010	-	1
STA 211+40	RT	0010	1	1
STA 211+40	LT	0010	-	1
STA 212+00	LT	0010	1	1
STA 212+00	RT	0010	-	1
STA 213+53	LT (MAPLE ST)	0010	-	1
STA 214+02	RT	0010	-	1
STA 214+03	LT	0010	-	1
STA 216+00	RT	0010	-	1
STA 216+05	LT	0010	-	1
STA 218+15	LT (ELM ST)	0010	-	1
STA 218+38	RT	0010	-	1
STA 218+47	LT (ELM ST)	0010	-	1
End Project	LT	0010	-	1
End Project	RT	0010	-	1
WATER MAIN EXTENSION		0010	-	1
UNDISTRIBUTED		0010	1	7
TOTALS			**3**	**23**

EROSION CONTROL MOBILIZATION & RESTORATION

LOCATION		CATEGORY	ITEM 618.0100 MAINTENANCE AND REPAIR OF HAUL ROADS (4100-31-71) (EACH)	ITEM 624.0100 WATER (MGAL)	ITEM 628.1905 MOBILIZATIONS EROSION CONTROL (EA)	ITEM 628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL (EA)	629.0205 FERTILIZER TYPE A (CWT)	630.0140 SEEDING MIXTURE NO. 40 (LB)	ITEM 631.0300 SOD WATER (MGAL)	ITEM 631.1000 SOD LAWN (SY)
211+13	to 218+69	0010	1	15	2	2	56	3	13	540
218+69	to 221+21	0010	-	-	-	-	1	1	1	9
UNDISTRIBUTED		0010	-	1	-	-	-	-	1	34
TOTALS			**1**	**16**	**2**	**2**	**57**	**4**	**15**	**583**

TRAFFIC CONTROL

	CATEGORY	DURATION CALANDAR DAYS	ITEM 643.0300 TRAFFIC CONTROL DRUMS #	(DAYS)	ITEM 643.0410 TRAFFIC CONTROL BARRICADES TYPE II #	(DAYS)	ITEM 643.0420 TRAFFIC CONTROL BARRICADES TYPE III * #	(DAYS)	ITEM 643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A * #	(DAYS)	ITEM 643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C #	(DAYS)	ITEM 643.0900 TRAFFIC CONTROL SIGNS #	(DAYS)
TOTAL PROJECT	0010	122	*32*	3904	*24*	*2928*	*20*	*2440*	*57*	6954	*9*	1098	*37*	4514
UNDISTRIBUTED	0010			70		*142*		*130*		306		120		96
		TOTALS		**3,974**		**3,070**		**2,570**		**7,260**		**1,218**		**4,610**

*ADDITIONAL ITEMS LISTED ELSEWHERE

3

TRAFFIC CONTROL DETOUR SIGN SUMMARY

CATEGORY 0010		SIGN	SIZE	NUMBER IN	APPROX. SERVICE PERIOD 122	643.3000 DETOUR SIGNS	643.0420 BARRICADES TYPE III *	643.0705 WARNING LIGHTS TYPE A *	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE	643.0920 COVERING SIGNS TYPE II	
SIGN NO.	LOCATION	CODE	W X H	SERVICE	DAYS	DAYS	DAYS	DAYS	DAYS	EACH	REMARKS
1	100' S OF MAPLE HEIGHTS ST ON HWY 32/57	W 20-1-G	48"x48"	1	122	122					
2A	IN FRONT OF J1-1 (JCT 151)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-R	21"x21"	1	122	122					
2B	RT OF SIGN # 2A	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-R	21"x21"	1	122	122					
3	MODIFY J 2-3 (S-151-AH; 32/57/AH; N-151-AH RT)	MO 4-8	24"x12"	1	122	122					
		MO 3-3	EXISTING								
		M 1-4	EXISTING								151
		MO 5-1-R	21"x21"	1	122	122					
4	MODIFY J 2-3 (S-151-AH; 32/57/AH; N-151-AH RT)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	EXISTING								32
	" " "	M 1-6	EXISTING								57
	" " "	MO 5-1-R	21"x21"	1	122	122					
5	D 1-3 (AH-GREEN BAY/FOND DU LAC; MANITOWOC-RT)									1	AH-GREEN BAY/ FOND DU LAC; ONE CYCLE
6	MODOFY J 2-1 (151-AH & RT)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	EXISTING								
	" " "	M 1-4	EXISTING								151
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
7	RT OF SIGN # 6	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
8	NE QUAD OF STH 32/57 (SOUTH LEG) & USH 151 (EAST LEG)	R 11-3	60"x30"	1	122	122	122	244			1/4 MILE
9	BELOW SIGN # 8	MO 4-9-R	30"x24"	1	122	122					
10	JV (SOUTH 151, NORTH-32/57)									1	SOUTH 151; NORTH-32/57; ONE CYCLE
11	J 3-3 (N-32-RT; N-57-RT; S-151-RT)									1	N-32-RT; N-57-RT; S-57-RT; ONE CYCLE
12	200' E OF STH 32/57 INTERSECTION ON USH 151 (EB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
13	RT OF SIGN # 12	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

3

TRAFFIC CONTROL DETOUR SIGN SUMMARY CONTINUED

CATEGORY 0010 SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 122 DAYS	643.3000 DETOUR SIGNS DAYS	643.0420 BARRICADES TYPE III * DAYS	643.0705 WARNING LIGHTS TYPE A * DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
14	D 1-1 (LT-MILWAUKEE; GREEN BAY/FOND DU LAC-RT)									1	GREEN BAY / FOND DU LAC-RT; ONE CYCLE
15	J 2-3 (S-57-AH LT; S-151-AH RT; N-57-AH RT)									1	S-151-AH RT; N-57-AH RT; ONE CYCLE
16	J 1-2 (JCT 32; JCT 57)	MO 4-8-A	24"x18"	2	122	244					
	" " "	M 50-2	EXISTING								32/57
17	100' E OF VOGT ST INTERSECTION ON USH 151 (EB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-L	21"x21"	1	122	122					
18	RT OF SIGN # 17	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-L	21"x21"	1	122	122					
19	@ BEGINNING OF LT TURN LANE ON HWY 151 EB	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
20	RT OF SIGN # 19	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
21	NE QUAD OF HWY 151 & IRISH RD INTERSECTION (FACING EB TRAFFIC)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
22	RT OF SIGN # 21	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
23	NW QUAD OF USH 151 & IRISH RD INTERSECTION	R 11-3	60"x30"	1	122	122	122	244			1 MILE
24	BELOW SIGN # 23	MO 4-9-R	30"x24"	1	122	122					
25	WITH SIGN # 21 (FACING WB TRAFFIC)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
26	WITH SIGN # 22 (FACING WB TRAFFIC)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

TRAFFIC CONTROL DETOUR SIGN SUMMARY CONTINUED

CATEGORY 0010											
SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 122 DAYS	643.3000 DETOUR SIGNS DAYS	643.0420 BARRICADES TYPE III * DAYS	643.0705 WARNING LIGHTS TYPE A * DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
27	SW QUAD OF USH 151 & IRISH RD INTERSECTION (FACING WB TRAFFIC)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
28	RT OF SIGN # 27	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
29	500' E OF SIGN # 25 ON USH 151 WB	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-R	21"x21"	1	122	122					
30	RT OF SIGN # 29	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-R	21"x21"	1	122	122					
31	400' E OF SIGN # 29 ON USH 151 WB	WO 20-2-A	48"x48"	1	122	122					
32	200' N OF HWY 151 INTERSECTION ON IRISH RD	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
33	RT OF SIGN # 32	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
34	200' W OF IRISH RD INTERSECTION ON HWY 151 (WB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
35	RT OF STOP SIGN (SB IRISH RD @ HWY 151)	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
36	RT OF SIGN # 35	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
37	500' N OF SIGN # 35 ON SB IRISH RD	MO 4-8-A	24"x18"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
38	RT OF SIGN # 37	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-R	21"x21"	1	122	122					

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

TRAFFIC CONTROL DETOUR SIGN SUMMARY CONTINUED

CATEGORY 0010											
SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 122 DAYS	643.3000 DETOUR SIGNS DAYS	643.0420 BARRICADES TYPE III * DAYS	643.0705 WARNING LIGHTS TYPE A * DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
39	200' N OF CLAY ST INTERSECTION ON IRISH RD	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
40	RT OF SIGN # 39	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
41	250' S OF MILL ST (NORTH LEG) ON IRISH RD	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-L	21"x21"	1	122	122					
42	RT OF SIGN # 41	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-L	21"x21"	1	122	122					
43	500' S OF BREED ST ON IRISH RD	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
44	RT OF SIGN # 43	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
45	RT OF STOP SIGN (IRISH RD & BREED ST INTERSECTION)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
46	RT OF SIGN # 45	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
47	NW QUAD OF IRISH RD & BREED ST INTERSECTION (FACING NB TRAFFIC)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
48	RT OF SIGN # 47	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					LEFT

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

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TRAFFIC CONTROL DETOUR SIGN SUMMARY CONTINUED

CATEGORY 0010											
SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 122 DAYS	643.3000 DETOUR SIGNS DAYS	643.0420 BARRICADES TYPE III * DAYS	643.0705 WARNING LIGHTS TYPE A * DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
49	250' W OF IRISH RD INTERSECTION ON BREED ST	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
50	RT OF SIGN # 49	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
51	NE QUAD OF BREED ST & IRISH RD INTERSECTION	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
52	RT OF SIGN # 51	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
53	SW QUAD OF BREED ST & IRISH RD INTERSECTION	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
54	RT OF SIGN # 53	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
55	500' W OF SIGN # 53 ON BREED ST (EB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-R	21"x21"	1	122	122					
56	RT OF SIGN # 55	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-R	21"x21"	1	122	122					
57	100' W OF SPRING ST ON BREED ST (WB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-L	21"x21"	1	122	122					
58	RT OF SIGN # 57	MO 4-8-A	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

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TRAFFIC CONTROL DETOUR SIGN SUMMARY CONTINUED

CATEGORY 0010											
SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 122 DAYS	643.3000 DETOUR SIGNS DAYS	643.0420 BARRICADES TYPE III * DAYS	643.0705 WARNING LIGHTS TYPE A * DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
59	RT OF STOP SIGN (BREED ST & HWY 32/57 INTERSECTION)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
60	RT OF SIGN # 57	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
61	200' E OF STH 32/57 INTERSECTION ON BREED ST (EB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
62	RT OF SIGN # 61	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					AHEAD
63	SE QUAD OF HWY 32/57 & E BREED ST INTERSECTION	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
64A	BACKSIDE OF SIGN # 63 (FACING NB TRAFFIC)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
64B	150' S OF SIGN # 64A ON HWY 32/57 (NB)	MO 4-5	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT
65A	NW QUAD OF HWY 32/57 & W BREED ST INTERSECTION	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
65B	150' N OF SIGN # 65A ON HWY 32/57 (SB)	MO 4-5	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	M 5-1-L	21"x21"	1	122	122					
66	BACKSIDE OF SIGN # 65 (FACING NB TRAFFIC)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 6-1	21"x21"	1	122	122					RIGHT

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

3

TRAFFIC CONTROL DETOUR SIGN SUMMARY CONTINUED

CATEGORY 0010											
SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 122 DAYS	643.3000 DETOUR SIGNS DAYS	643.0420 BARRICADES TYPE III * DAYS	643.0705 WARNING LIGHTS TYPE A * DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
67A	SW QUAD OF STH 32/57 & BREED ST INTERSECTION	R 11-3	60"x30"	1	122	122	122	244			1/4 MILE
67B	BELOW SIGN # 67A	MO 4-9-R	30"x24"	1	122	122					
68	J4-2 (S-32; S-57)									1	S-32, S-57; ONE CYCLE
69A	500' N OF SIGN # 65 ON HWY 32/57 (SB)	MO 4-5	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					
	" " "	M 5-1-L	21"x21"	1	122	122					
69B	RT OF SIGN # 69A ON HWY 32/57 (SB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-L	21"x21"	1	122	122					
70	500' N OF SIGN # 69 ON HWY 32/57 (SB)	WO 20-2-A	48"x48"	1	122	122					
71A	500' S OF SIGN # 64 ON HWY 32/57 (NB)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-R	21"x21"	1	122	122					
71B	150' S OF SIGN # 71A ON HWY 32/57 (NB)	MO 4-5	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-R	21"x21"	1	122	122					
72	J2-3 (N-151; S-32; S-57; AH)									1	S-32, S-57; ONE CYCLE
73	MODIFY J2-3 (N-151; S-32; S-57)	MO 4-8-A	24"x18"	1	122	122					
	" " "	M 1-4	EXISTING								
74	ABOVE J4-2 (N-32, N-57)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
75	D 1-3 (AH- MILWAUKEE/MANITOWOC; FOND DU LAC-RT)									1	AH-MILWAUKEE / MANITOWOC; ONE CYCLE
76	SW QUAD OF STH 32/57 & HWY 151 INTERSECTION	R 11-3	60"x30"	1	122	122	122	244			1/4 MILE
77	MODIFY J2-2 (CTY Y-AH; 151-RT)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	EXISTING								
	" " "	M 1-4	EXISTING								151
	" " "	MO 6-1	21"x21"	1	122	122					LEFT
78	MODIFY J2-1 (32/57-LT & RT)	MO 4-5	24"x12"	1	122	122					
	" " "	M 1-6B	EXISTING								32
	" " "	M 1-6	EXISTING								57
	" " "	MO 6-1	21"x21"	1	122	122					LEFT

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

3

TRAFFIC CONTROL DETOUR SIGN SUMMARY CONTINUED

CATEGORY 0010											
SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 122 DAYS	643.3000 DETOUR SIGNS DAYS	643.0420 BARRICADES TYPE III * DAYS	643.0705 WARNING LIGHTS TYPE A * DAYS	643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	643.0920 COVERING SIGNS TYPE II EACH	REMARKS
79	ABOVE J1-1 (JCT Y)	MO 4-8	24"x12"	1	122	122					
	" " "	MO 3-1	24"x12"	1	122	122					
	" " "	M 1-4	24"x24"	1	122	122					151
	" " "	MO 5-1-L	21"x21"	1	122	122					
80A	150' S OF SIGN # 79 ON HWY 151 (EB)	MO 4-5	24"x12"	1	122	122					
	" " "	MO 3-3	24"x12"	1	122	122					
	" " "	M 1-6B	24"x24"	1	122	122					32
	" " "	M 1-6	24"x24"	1	122	122					57
	" " "	MO 5-1-L	21"x21"	1	122	122					
80B	150' W OF SIGN # 79 ON HWY 151 (EB)	WO 20-2-A	48"x48"	1	122	122					
81	J 4-3 (N-151; S-32; S-57)									1	N-151; S-32; S-57; ONE CYCLE
82	J 4-3 (N-151; S-32; S-57; AH LT)									1	N-151; S-32; S-57, AH LT; ONE CYCLE
83	J 4-3 (N-151; S-32; S-57; LT)									1	N-151; S-32; S-57, LT; ONE CYCLE
84	J2-2 (N-151-LT; G-AH)									1	N-151-LT; ONE CYCLE
85	FIELD DETERMINER (FACING SB TRAFFIC)			1					7		
86	FIELD DETERMINER (FACING NB TRAFFIC)			1					7		
	TOTAL			**293**		**35,746**	**488**	**976**	**14**	**12**	

CONTINUED FROM PREVIOUS SHEET

* ADDITIONAL ITEMS LISTED ELSEWHERE

3

PAVEMENT MARKING

LOCATION			CATEGORY	ITEM 646.0106 PAVEMENT MARKING EPOXY 4-INCH YELLOW (LF)	ITEM 647.0566 PAVEMENT MARKING STOP LINE EPOXY 18-INCH WHITE (LF)	ITEM 647.0776 PAVEMENT MARKING CROSSWALK EPOXY 12-INCH WHITE (LF)
211+14	TO	218+70	0010	1,253	-	-
INTERSECTION OF MAPLE ST AND E CHESTNUT			0010	-	30	290
INTERSECTION OF ELM ST AND E CHESTNUT			0010	-	15	220
UNDISTRIBUTED			0010	17	5	10
TOTALS				**1,270**	**50**	**520**

CONSTRUCTION STAKING ITEMS

LOCATION			CATEGORY	ITEM 650.4000 CONSTRUCTION STAKING STORM SEWER (EACH)	ITEM 650.4500 CONSTRUCTION STAKING SUBGRADE (LF)	ITEM 650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER (LF)	ITEM 650.7000 CONSTRUCTION STAKING CONCRETE PAVEMENT (LF)	ITEM 650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (4100-31-71) (LS)	ITEM SPV.0105.02 CONSTRUCTION STAKING WATER MAIN (LS)
211+13	to	218+69	0010	14	756	-	756	1	-
218+69	to	221+21	0010	-	-	45	-	-	-
218+69	to	221+21	0020	-	-	-	-	-	1
UNDISTRIBUTED			0010	-	75	20	75	-	-
TOTALS				**14**	**831**	**65**	**831**	**1**	**1**

SAWING

LOCATION			CATEGORY	ITEM 690.0150 SAWING ASPHALT (LF)	ITEM 690.0250 SAWING CONCRETE (LF)
211+13	to	218+69	0010	923	272
218+69	to	221+21	0010	540	10
UNDISTRIBUTED			0010	22	23
TOTALS				**1,485**	**305**

3

WATER MAIN ITEMS

LOCATION			CATEGORY	ITEM SPV.0060.01 ADJUSTING WATER VALVE BOXES (EA)	ITEM SPV.0060.02 ABANDON WATER VALVE (EA)	ITEM SPV.0060.03 REMOVE FIRE HYDRANT (EA)	ITEM SPV.0060.04 CUT AND PLUG EXISTING WATER MAIN 6-INCH (EA)	ITEM SPV.0060.05 TEE 8" X 6" (EA)	ITEM SPV.0060.06 BEND 45-DEGREE 8" (EA)	ITEM SPV.0060.07 BEND 45-DEGREE 6" WITH 8" X 6" REDUCER (EA)	ITEM SPV.0060.08 REMOVE WATER SERVICE CURB STOP (EA)	ITEM SPV.0060.09 WATER SERVICE CURB STOP (EA)	ITEM SPV.0060.10 CONNECT TO EXISTING WATER MAIN (EA)	ITEM SPV.0060.11 WATER VALVE AND BOX 6-INCH (EA)	ITEM SPV.0060.12 WATER VALVE AND BOX 8-INCH (EA)
211+13	to	218+69	0020	1	1	1	1	1	1	1	8	7	2	1	-
218+69	to	221+21	0020	-	-	-	1	-	1	1	3	3	1	-	1
UNDISTRIBUTED			0020	-	-	-	-	-	8	-	-	-	-	-	-
TOTALS				**1**	**1**	**1**	**2**	**1**	**10**	**2**	**11**	**10**	**3**	**1**	**1**

WATER MAIN ITEMS CONTINUED

LOCATION			CATEGORY	ITEM SPV.0090.01 WATER MAIN 6-INCH PVC (LF)	ITEM SPV.0090.02 WATER MAIN 8-INCH (LF)	ITEM SPV.0090.03 WATER SERVICE 1-INCH (LF)	ITEM SPV.0090.04 REMOVING ASBESTOS WATER MAIN * (LF)	ITEM SPV.0105.03 TEMPORARY WATER (LS)	ITEM 203.0210.S.01 ABATEMENT OF ASBESTOS CONTAINING MATERIAL WATER MAIN* (LS)
211+13	to	218+69	0020	7	760	249	25	1	1
218+69	to	221+21	0020	3	256	68	-	-	-
UNDISTRIBUTED			0020	-	14	8	-	-	-
TOTALS				**10**	**1,030**	**325**	**25**	**1**	**1**

*** APPROXIMATE QUANTITY OF 25 LF OF ABATEMENT OF ASBESTOS WATER MAIN AND REMOVING ASBESTOS WATER MAIN ASSUMED FOR ITEMS**

CONVENTIONAL SIGNS AND ABBREVIATIONS

COUNTY LINE
TOWNSHIP AND RANGE LINES
SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W LINE
PROPERTY LINE
CORPORATE LIMITS — NAME
LOT, TIE AND OTHER MINOR LINES
SLOPE INTERCEPTS
UNDERGROUND FACILITY (GAS, TELEPHONE, ELECTRIC, ETC.) — C (TYPE)
FENCE
HAZARDOUS UTILITY SITE — CAUTION (TYPE)
CULVERT (BOX, PIPE OR CATTLE PASS)
RAIL LINE
FOUNDATION OR RUIN — FDN.
BUILDING — TYPE
CEMETERY — CEMETERY
TELEPHONE PEDESTAL
IRON PIN — IP
IRON PIN SET (TYPE 2)
VALVE — VLV (TYPE)
SILO, MANHOLE, VENT SEPTIC VENT, WELL, ETC. — MH (TYPE)

	NON-COMPENSABLE	COMPENSABLE
SERVICE PEDESTAL		
POWER POLE		
TELEPHONE POLE		
SIGN		

ACCESS RESTRICTED (BY ACQUISITION)
NO ACCESS (BY STATUTORY AUTHORITY)
ACCESS RESTRICTED (BY PREVIOUS PROJECT)

PERMANENT LIMITED EASEMENT
TEMPORARY LIMITED EASEMENT
FEE
NON-MONUMENTED SURVEY POINT
SECTION CORNER (T 5 N, R 7 E; 5, 4, 8, 9)
BUSHES (DECIDUOUS)
TREES (CONIFEROUS)
WOODS

NOTES:

BEARINGS ON THIS PLAT ARE ORIENTED TO GRID NORTH OF THE WISCONSIN COUNTY COORDINATE SYSTEM, CALUMET COUNTY, NAD83 (1991), WITHIN WHICH THE WEST LINE OF THE SW 1/4 OF SECTION 18, T18N, R20E IS ASSUMED TO BEAR N00°33'58"W.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, CALUMET COUNTY , NAD 83(1991) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY 3/4" X 24" IRON PIPE) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES HEREON ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM AND/OR OTHER SURVEYS OF PUBLIC RECORD.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL/MONUMENTED LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING OWNERSHIP LINES, EXCLUDING RIGHT OF WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE WISCONSIN DEPARTMENT OF TRANSPORTATION NE REGION OFFICE. IN GREEN BAY, WI.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENTS

E. CHESTNUT STREET, CITY OF CHILTON

S. MADISON STREET - ELM STREET

U.S.H. 151

CALUMET

STATE PROJECT NUMBER
4100-31-21

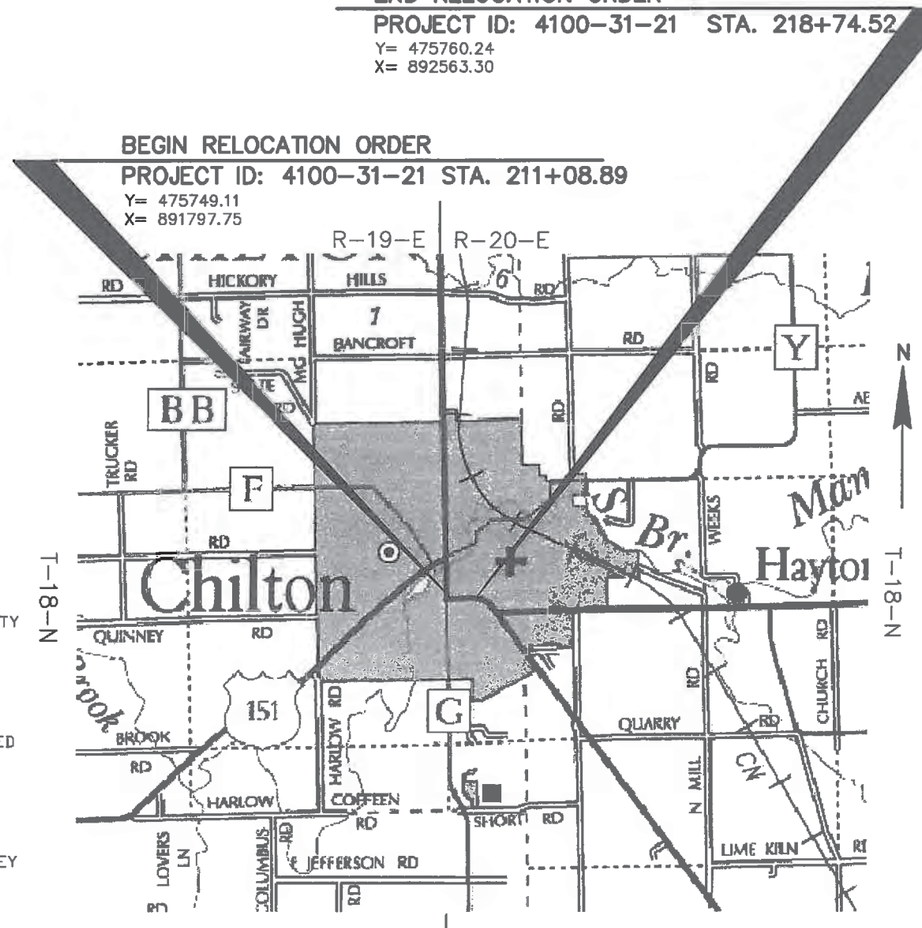
R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
4100-31-21	4.1	4
FEDERAL PROJECT NUMBER		

PLAT OF RIGHT-OF-WAY REQUIRED FOR
E. CHESTNUT STREET – U.S.H. "151"
CITY OF CHILTON
U.S.H. 151 (E. CHESTNUT ST.) CALUMET COUNTY

CONSTRUCTION PROJECT NUMBER
4100-31-71

END RELOCATION ORDER
PROJECT ID: 4100-31-21 STA. 218+74.52
Y= 475760.24
X= 892563.30

BEGIN RELOCATION ORDER
PROJECT ID: 4100-31-21 STA. 211+08.89
Y= 475749.11
X= 891797.75



TOTAL NET LENGTH OF CENTERLINE = 0.143 MI. (URBAN)

AC.	ACRE	LT.	LEFT
AH.	AHEAD	MI.	MILE
ALUM. MON.	ALUMINUM MONUMENT	PG	PAGE
ANT.	ANTENNA	P.C.	POINT OF CURVATURE
A.P.	ACCESS POINT	P.E.	PRIVATE ENTRANCE
B	BARN	PERM.	PERMANENT
BK.	BACK	P.I.	POINT OF INTERSECTION
B.M.	BENCH MARK	P.T.	POINT OF TANGENCY
C	CHURCH	P.L.E.	PERMANENT LIMITED EASEMENT
C.E.	COMMERCIAL ENTRANCE	R.	RADIUS
C/L	CENTERLINE	R.D.E.	RESTRICTED DEVELOPMENT EASEMENT
CONC. MON.	CONCRETE MONUMENT	REM.	REMAINING
CONST.	CONSTRUCTION	REST.	RESTAURANT
C.P.	CULVERT PIPE	RT.	RIGHT
C.S.M.	CERTIFIED SURVEY MAP	R/W	RIGHT OF WAY
D.	DEGREE OF CURVE	S.	SHED
ETAL	AND OTHERS	S.D.	STORM DRAINAGE
F.E.	FIELD ENTRANCE	S.F.	SQUARE FEET
FRL.	FRACTIONAL	STA.	STATION
FT.	FEET	T	TANK
G	GARAGE	TAV.	TAVERN
H	HOUSE	TEMP.	TEMPORARY
L.	LENGTH OF CURVE	T.L.E.	TEMPORARY LIMITED EASEMENT
L.C.	LONG CHORD OF CURVE	VOL.	VOLUME
L.C.B.	LONG CHORD BEARING	W	WALL

APPROVED FOR THE CITY OF CHILTON
9-29-14 Todd Schug
DATE SIGNATURE

ORIGINAL PLAT PREPARED BY:
ONE SOURCE CONSULTING

REVISION DATE

SEPTEMBER 25, 2014 James R. Braty
DATE REGISTERED LAND SURVEYOR

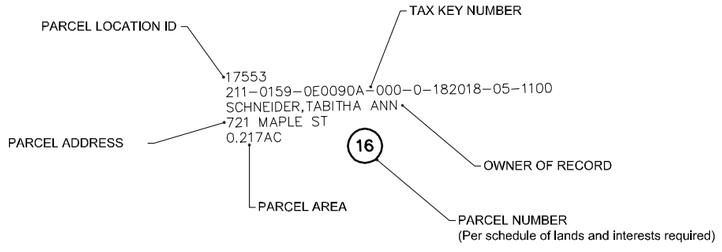
E

FILE NAME : Z:\PROJECTS\STH 151 Chilton\4100-31-21_RW_TITLE.DWG PLOT DATE : 9/25/2014 10:00 AM PLOT BY : CDONZE PLOT NAME : WISDOT/CADDS

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER	INTEREST REQUIRED	TOTAL (ACRES)	T.L.E. REQ'D. (SQ. FT.)	P.L.E. REQ'D. (SQ. FT.)
1	GREGOR PROPERTIES LLC	T.L.E.	1.64	60.47	---
2	CONVENIENCE STORE INVESTMENTS	T.L.E.	2.47	1514.51	---
4	CHESTNUT STREET PROPERTIES INC	T.L.E.	1.499	1320.95	---
5	CITY OF CHILTON (MAPLE ST. R.O.W)	---	0.88	---	---
6	GREGOR PROPERTIES LLC	T.L.E.	0.22	214.50	---
7	MUELLER, ERIK J	T.L.E. & P.L.E.	0.281	164.29	124.65
8	HILBERT, WILLIAM E & JULIE A	T.L.E.	0.525	539.50	---
9	PRODUCTIONCREDIT ASSN OF EAST	T.L.E.	0.31	682.50	---
10	SCHLAEFER REVOC LIVING TRUST	T.L.E.	0.327	202.88	---
11	WEBER OIL COMPANY	T.L.E.	0.313	736.46	---
12	LA-TEC CREDIT UNION	T.L.E.	0.668	884.39	---
14	PREMIER FINANCIAL CREDIT UNION	T.L.E.	0.840	1179.78	---
17	WHALEY, JAMES A	T.L.E. & P.L.E.	0.778	1485.32	152.70
100	WISCONSIN PUBLIC SERVICE	RELEASE OF RIGHTS	---	---	---

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS.



PARCEL LABEL LEGEND

COURSE TABLE – T.L.E.			
FROM	TO	BEARING	DIST.
100	108	N 89°13'58" E	24.20'
108	115	S 00°46'02" E	6.50'
115	101	S 89°13'58" W	24.20'
101	100	N 00°46'02" W	6.50'
106	107	N 89°13'58" E	1.13'
107	103	S 00°33'58" E	6.50'
103	114	S 89°13'58" W	1.11'
114	106	N 00°46'02" W	6.50'
107	109	N 89°13'58" E	105.00'
109	112	S 00°33'58" E	6.50'
112	103	S 89°13'58" W	105.00'
109	110	N 89°13'58" E	83.00'
110	111	S 00°33'58" E	6.50'
111	112	S 89°13'58" W	83.00'
113	122	N 89°08'03" E	233.00'
122	121	S 00°33'58" E	6.50'
121	116	S 89°08'03" W	233.00'
116	113	N 00°33'58" W	6.50'
122	123	N 89°08'03" E	203.22'
123	124	S 00°41'28" E	6.50'
124	121	S 89°08'03" W	203.23'
123	125	N 89°08'03" E	33.00'
125	126	S 00°33'58" E	6.50'
126	124	S 89°08'03" W	33.00'
125	128	N 89°08'03" E	9.29'
128	127	S 00°51'57" E	6.50'
127	126	S 89°08'03" W	9.31'
134	133	N 00°33'58" W	20.00'
133	132	N 89°08'03" E	7.00'
132	131	S 00°33'58" E	13.00'
131	130	N 89°08'03" E	8.96'
130	129	S 00°51'57" E	7.00'
129	134	S 89°08'03" W	16.00'
139	140	N 00°33'58" W	6.50'
140	137	N 89°08'03" E	98.30'
137	136	N 00°33'58" W	13.00'
136	135	N 89°08'03" E	5.00'
135	138	S 00°33'58" E	19.50'
138	139	S 89°08'03" W	103.30'
139	143	S 89°08'03" W	136.09'
143	144	N 00°33'58" W	6.50'
144	140	N 89°08'03" E	136.09'
144	143	S 00°33'58" E	6.50'
143	147	S 89°08'03" W	173.81'
147	148	N 00°33'58" W	16.50'
148	149	N 89°08'03" E	5.00'
149	150	S 00°33'58" E	10.00'
150	144	N 89°08'03" E	168.81'
155	160	N 00°46'02" W	6.50'
160	154	N 89°13'58" E	132.31'
154	153	N 00°33'58" W	9.00'
153	151	N 89°13'58" E	5.00'
151	152	S 00°33'58" E	15.50'
152	155	S 89°13'58" W	137.29'
162	163	N 00°46'02" W	6.50
163	158	N 89°13'58" E	85.37'
158	157	S 00°46'02" E	6.50
157	162	S 89°13'58" W	84.18'

COURSE TABLE – P.L.E.			
FROM	TO	BEARING	DIST.
157	159	N 00°46'02" W	11.00'
159	161	N 89°13'58" E	13.88'
161	155	S 00°46'02" E	11.00'
155	157	S 89°13'58" W	13.88'
108	106	N 89°13'58" E	13.85'
106	105	S 00°46'02" E	9.00'
105	102	S 89°13'58" W	13.85'
102	108	N 00°46'02" W	9.00'

4

4

REVISION DATE

DATE SEPTEMBER 12, 2014

SCALE IN FEET 0 20 40

HWY: U.S.H. 151

COUNTY: CALUMET

STATE R/W PROJECT NUMBER 4100-31-21

CONSTRUCTION PROJECT NUMBER 4100-31-71

PLAT SHEET 4.02

PS&E SHEET 56

E

FILE NAME : 4100-31-21_rw_sht.dwg

PLOT DATE : 9/12/2014 10:00 AM PLOT BY : CDONZE PLOT NAME :

WISDOT/CADDS SHEET 70

PLE (PERMANENT LIMITED EASEMENT) AS SHOWN FOR STORM DRAINAGE PURPOSES
TLE (TEMPORARY LIMITED EASEMENT) AS SHOWN FOR SLOPE GRADING PURPOSES

CITY

OF

CHILTON

BEGIN RELOCATION ORDER
STA. 211+08.89
Y = 475749.11
X = 891797.75

PK NAIL
Y = 477725.76
X = 891669.52

2" IRON PIPE
Y = 475090.27
X = 891695.55

T 18 N
R 19 E
13 18
13 18
24 19

U.S.H. "151"
N00°33'58"W 1978.22'
W. LINE SW 1/4 SECTION 18-18-19
N00°33'58"W 2635.63'

C.T.H. G
S00°33'58"E 657.42'

MAPLE ST.

E. CHESTNUT ST.

W. CHESTNUT ST.

R U.S.H. "151" & CONSTRUCTION ℄

210+00.18
211+00 POB
212+00
213+00
214+00
215+00

N89°13'58"E 108.71'
N89°13'58"E 257.57'

PI: 213+66.46

48.75'

R.O.W. SHOWN PER DOC #372772

KERSTEN'S SUBDIVISION OF N. HALF GOV'T. LOT 11
BLOCK "E"

KERSTEN'S SUBDIVISION OF N. HALF GOV'T. LOT 11
BLOCK "D"

SHED

EXIST HSE

EXIST BLDG

GARAGE

LOT 9
LOT 10
LOT 11
LOT 12
LOT 15
LOT 16
LOT

17555
211-0159-0E0120A-000-0-182018-05-1100
WHALEY,JAMES A
718 S MADISON ST
0.296AC

17554
211-0159-0E0110A-000-0-182018-05-1100
WHALEY,JAMES A
32 E CHESTNUT ST
0.186AC

17544
211-0159-0D0150A-000-0-182018-05-1100
PREMIER,FINANCIAL CREDIT UNION
40 E CHESTNUT ST
0.313AC

17084
211-0340-00L0100-000-0-182018-05-110A
MUELLER,ERIC J
804 S MADISON ST
0.28AC

17087
211-0340-00L0100-000-0-182018-05-110D
PRODUCTION,CREDIT ASSN OF EAST
23 E CHESTNUT ST
0.31AC

17085
211-0340-00L0100-000-0-182018-05-110B
HILBERT,WILLIAM E & JULIE A
31 E CHESTNUT ST
0.525AC

17012
211-0340-00L0600-000-0-182018-05-110E
CITY,OF CHILTON
MAPLE ST
0.88AC

17010
211-0340-00L0500-000-0-182018-05-110A
CONVENIENCE,STORE INVESTMENTS
45 E CHESTNUT ST
0.93AC

LOT 1
CSM 1327

PART OF OUTLOT 1
GOVT LOT 11
ASSESSORS PLAT OF
THE CITY OF CHILTON

PART OF OUTLOT 6

E. LINE OUTLOT 1

W. LINE OUTLOT 1

6' Q.C.D. FROM CITY
DOC #223030

GAS
RELEASE OF RIGHTS

SLOPE INTERCEPT (TYP)

T.L.E.
P.L.E.
P.L.

59.6'
91.4'
63.3'
58.0'
46.3'
28.2'
30.1'
0.88'

66'
60'

REVISION DATE	DATE SEPTEMBER 12, 2014	SCALE IN FEET 0 20 40	HWY: U.S.H. 151	STATE R/W PROJECT NUMBER 4100-31-21	PLAT SHEET 4.03
			COUNTY: CALUMET	CONSTRUCTION PROJECT NUMBER 4100-31-71	PS&E SHEET 57 E

FILE NAME : 4100-31-21_rw_sht.dwg PLOT DATE : 9/12/2014 10:00 AM PLOT BY : CDONZE PLOT NAME :

WISDOT/CADDS SHEET 70

PLE (PERMANENT LIMITED EASEMENT) AS SHOWN FOR STORM DRAINAGE PURPOSES
TLE (TEMPORARY LIMITED EASEMENT) AS SHOWN FOR SLOPE GRADING PURPOSES

CITY

OF

CHILTON

4

4

N

BLOCK SHORT – LOTS PRORATED

48.75'
PLAT

KERSTEN'S SUBDIVISION OF
N. HALF GOV'T. LOT 11
BLOCK "D"

KERSTEN'S SUBDIVISION OF
N. HALF GOV'T. LOT 11
BLOCK "C"

LOT 15 | LOT 14 | LOT 13 | LOT 12 | LOT 11 | LOT 10 | LOT 9 | LOT 16 | LOT 15 | LOT 14

℄ U.S.H. "151" &
CONSTRUCTION ℄

SLOPE INTERCEPT
(TYP)

T.L.E.

P.L.

ELM ST.

17543
211–0159–0D0140A–000–0–182018–05–1100
PREMIER,FINANCIAL CREDIT UNION
46 E CHESTNUT ST
0.214

17542
211–0159–0D0120A–000–0–182018–05–1100
LA–TEC,CREDIT UNION
50 E CHESTNUT ST
0.256AC

17541
211–0159–0D0110A–000–0–182018–05–1100
LA–TEC,CREDIT UNION
56 E CHESTNUT ST
0.156AC

17540
211–0159–0D0090A–000–0–182018–05–1100
WEBER,OIL COMPANY
64 E CHESTNUT ST
0.313AC

17533
211–0159–0C0150A–000–0–182018–05–1100
SCHLAEFER,REVOC LIVING TRUST
102 E CHESTNUT ST
0.327AC

17532

N89°08'03"E 508.06'

E. CHESTNUT ST.

U.S.H. "151"

215+00 | 216+00 | 217+00 | 218+00 | 219+00 | 220+00

144 215+66.89 39.50
143 215+66.86 33.00
140 217+02.98 39.50
139 217+02.95 33.00
136 218+01.35 52.50
137 218+01.28 39.50
138 218+06.25 33.00
135 218+06.35 52.50
134 218+56.00 33.00
133 218+55.11 53.00
132 218+62.11 53.00
131 218+62.04 40.00
130 218+71.00 40.00
129 218+71.00 33.00
122 216+29.07 33.00
121 216+28.98 39.50
123 218+32.23 33.00
124 218+32.21 39.50
125 218+85.23 33.00
126 218+85.21 39.50
128 218+74.52 33.00
127 218+74.52 39.50

33'

W | G | FO | OH | 8 SAN | 6 SAN | 24 SS | M | 24 SS

LOT 1
CSM 1327
2
–00L0500–000–0–182018–05–110A
NCE,STORE INVESTMENTS
STNUT ST

LOT 1
CSM 1607
2
17009
211–0340–00L0600–000–0–182018–05–110C
CONVENIENCE,STORE INVESTMENTS
51 E CHESTNUT ST
0.61AC

INGRESS–EGRESS
EASEMENT PER CSM

LOT 2
CSM 1607
4
17011
211–0340–00L0600–000–0–182018–05–110D
CHESTNUT,STREET PROPERTIES INC
61 E CHESTNUT ST
1.499AC

15' UTILITY
EASEMENT
PER CSM

17015
211–0340–00L0600–000–0–182018–05–110H
GREGOR,PROPERTIES LLC
0.22
6

1
17007
211–0340–00L0600–000–0–182018–05–110A
GREGOR,PROPERTIES LLC
101 E CHESTNUT ST
1.64AC

END RELOCATION ORDER
STA. 218+74.52
Y = 475760.24
X = 892563.30

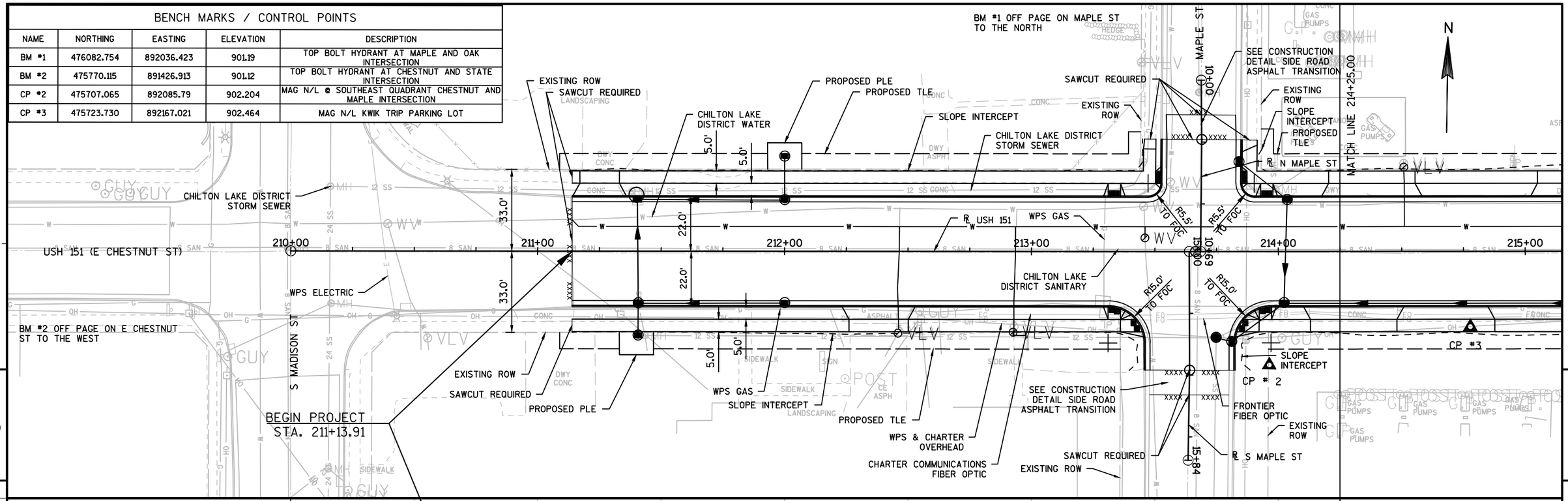
PART OF OUTLOT 6
PART GOVT LOT 11
ASSESSORS PLAT OF
THE CITY OF CHILTON

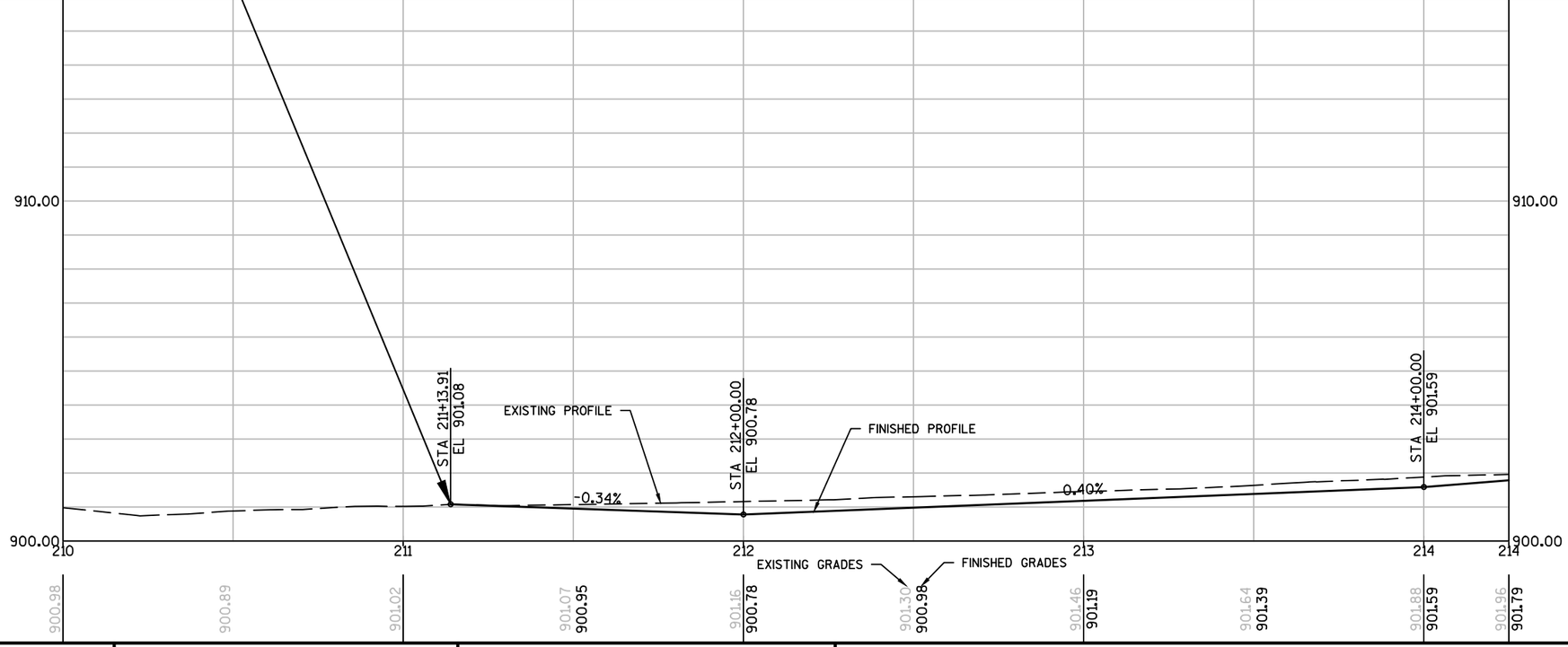
REVISION DATE	DATE SEPTEMBER 12, 2014	SCALE IN FEET 0 20 40	HWY: U.S.H. 151	STATE R/W PROJECT NUMBER 4100–31–21	PLAT SHEET 4.04
			COUNTY: CALUMET	CONSTRUCTION PROJECT NUMBER 4100–31–71	PS&E SHEET 58 E

FILE NAME : 4100–31–21_rw_sht.dwg PLOT DATE : 9/12/2014 10:00 AM PLOT BY : CDONZE PLOT NAME :

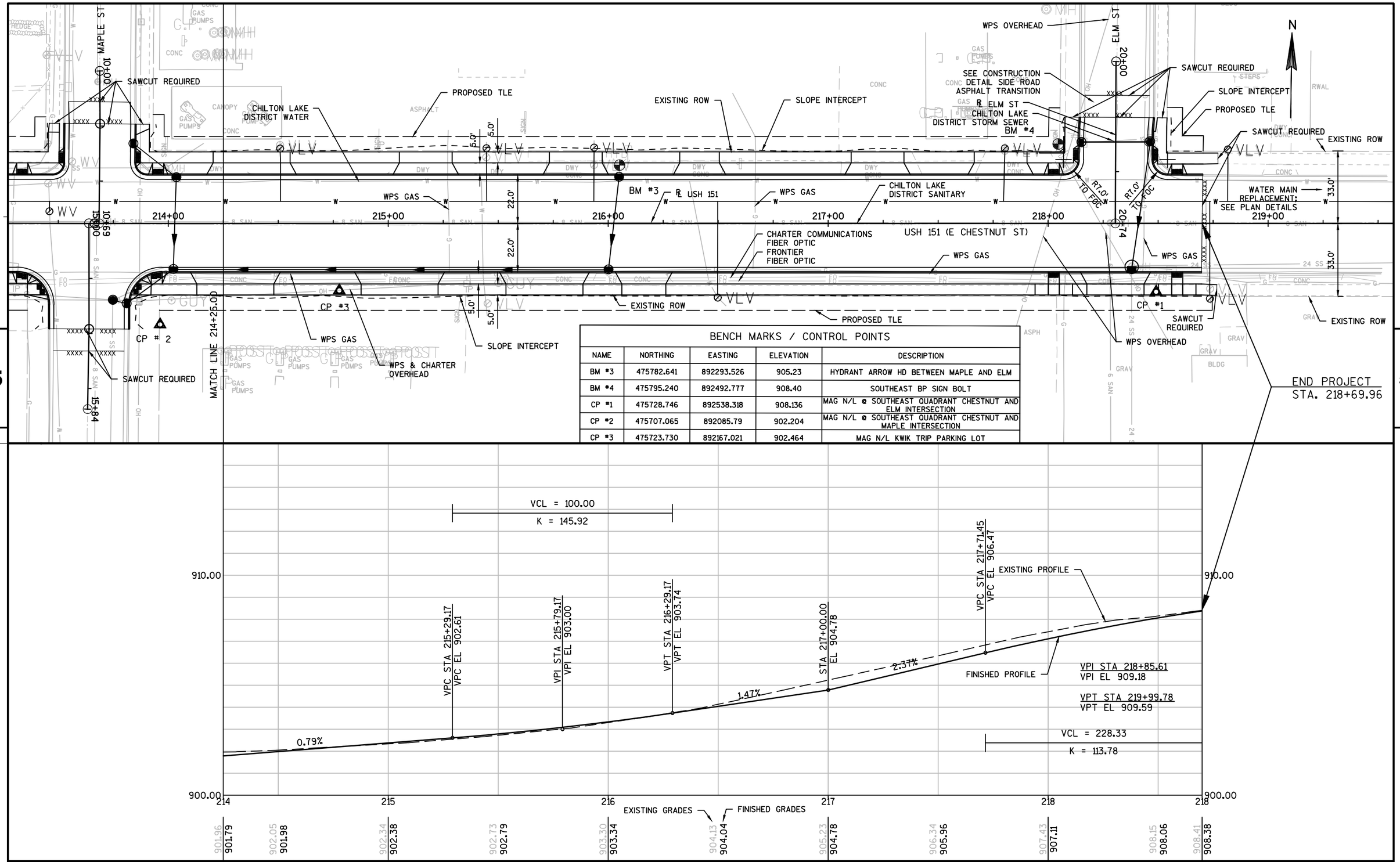
WISDOT/CADDS SHEET 70

BENCH MARKS / CONTROL POINTS				
NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION
BM #1	476082.754	892036.423	901.19	TOP BOLT HYDRANT AT MAPLE AND OAK INTERSECTION
BM #2	475770.115	891426.913	901.12	TOP BOLT HYDRANT AT CHESTNUT AND STATE INTERSECTION
CP #2	475707.065	892085.79	902.204	MAG N/L @ SOUTHEAST QUADRANT CHESTNUT AND MAPLE INTERSECTION
CP #3	475723.730	892167.021	902.464	MAG N/L KWIK TRIP PARKING LOT





PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PLAN AND PROFILE: USH 151	SHEET 59	E



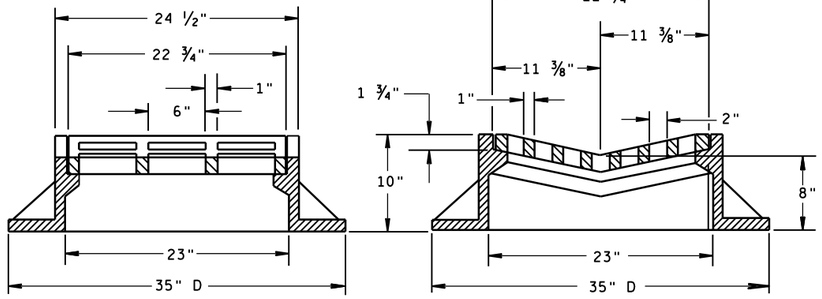
BENCH MARKS / CONTROL POINTS

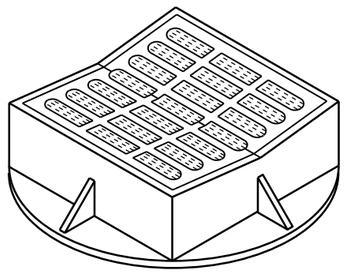
NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION
BM #3	475782.641	892293.526	905.23	HYDRANT ARROW HD BETWEEN MAPLE AND ELM
BM #4	475795.240	892492.777	908.40	SOUTHEAST BP SIGN BOLT
CP #1	475728.746	892538.318	908.136	MAG N/L @ SOUTHEAST QUADRANT CHESTNUT AND ELM INTERSECTION
CP #2	475707.065	892085.79	902.204	MAG N/L @ SOUTHEAST QUADRANT CHESTNUT AND MAPLE INTERSECTION
CP #3	475723.730	892167.021	902.464	MAG N/L KWIK TRIP PARKING LOT

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	PLAN AND PROFILE: USH 151	SHEET 60	E

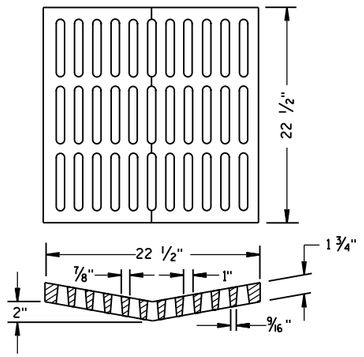
Standard Detail Drawing List

08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-19C	INLET COVERS TYPE F, HM, HM-S, S, T, V, HM-GJ, & HM-GJ-S
08A08-01	CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER
08D01-17	CONCRETE CURB, CONCRETE CURB AND GUTTER AND TIES
08D05-15A	CURB RAMPS TYPES 1 AND 1-A
08D05-15B	CURB RAMPS TYPES 2 AND 3
08D05-15C	CURB RAMPS TYPES 4A AND 4A1
08D05-15D	CURB RAMPS TYPE 4B AND 4B1
08D05-15E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13C01-17	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
13C13-08	URBAN DOWELED CONCRETE PAVEMENT
13C18-02A	CONCRETE PAVEMENT JOINTING
13C18-02B	CONCRETE PAVEMENT STEEL REINFORCEMENT
13C18-02C	CONCRETE PAVEMENT JOINT TIES
13C18-02D	CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES
15C02-05A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C33-01	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D20-03	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D30-02A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-02B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-02C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



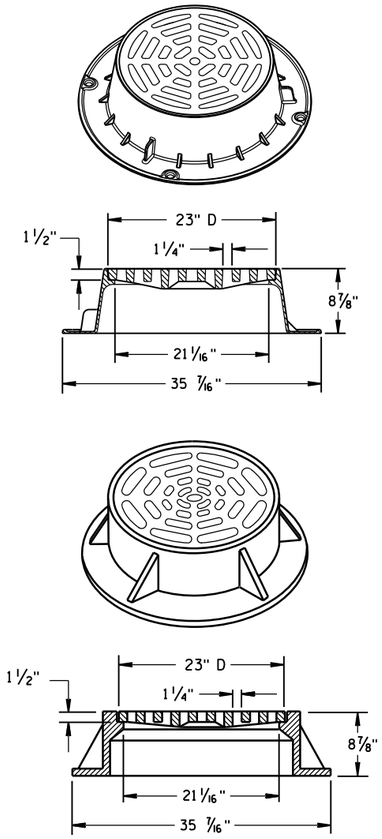


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

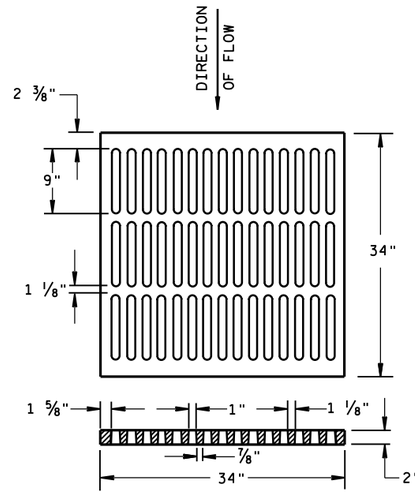
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

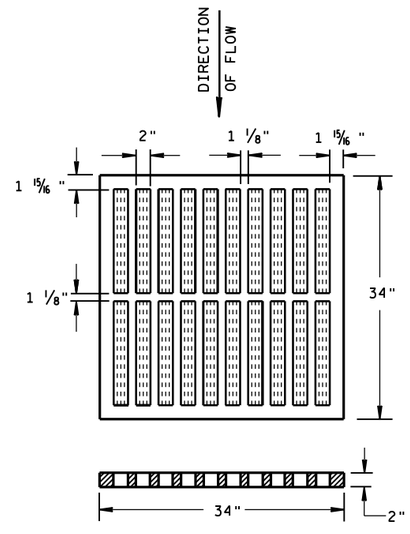
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



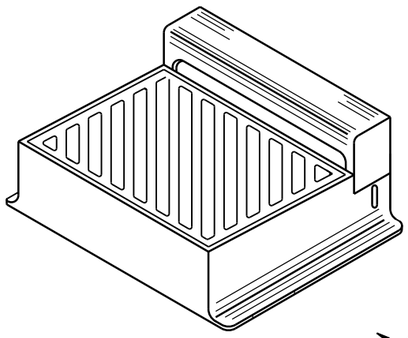
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

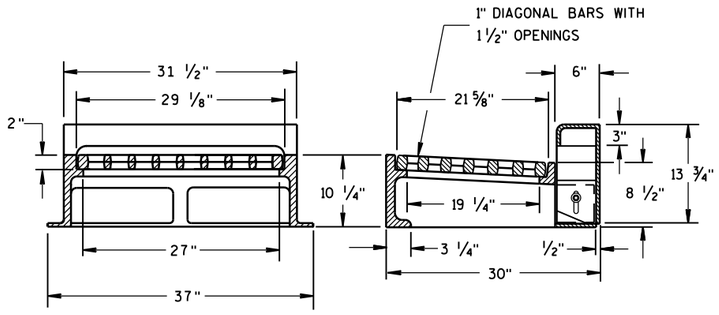


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

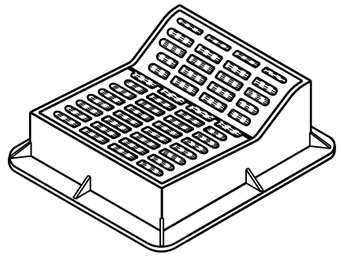
INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM

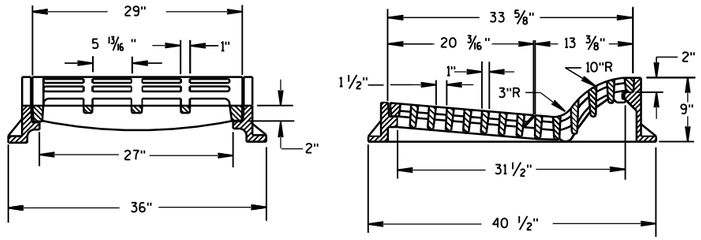
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

6

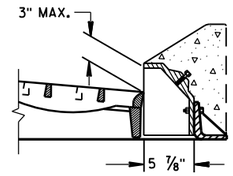
S.D.D. 8 A 5-19b





TYPE "F"

USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.



ALTERNATIVE CURB BOX FOR TYPE "HM" COVER

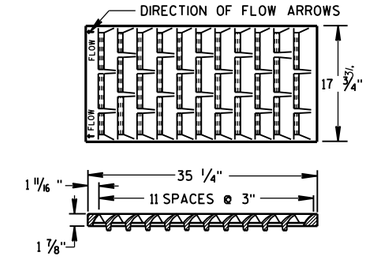
USE WITH TYPES G & J CONCRETE CURB & GUTTER, 30 INCH
NOTED AS TYPE HM-GJ ON DRAINAGE TABLE

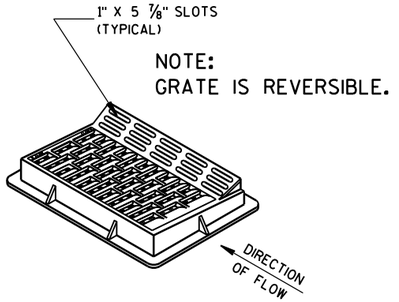
NOTE:
SPECIAL GRATE FOR THE
TYPE "H" COVER MAY ALSO BE
USED FOR THE TYPE "HM-GJ" COVER
NOTED AS TYPE HM-GJ-S ON DRAINAGE TABLE

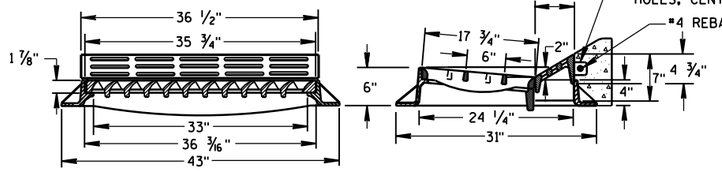
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.



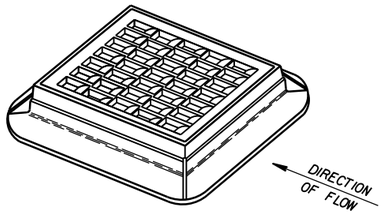


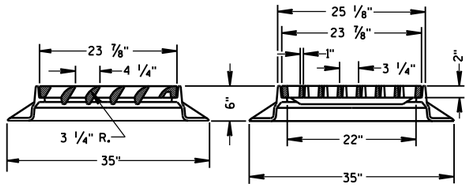


TYPE "HM"

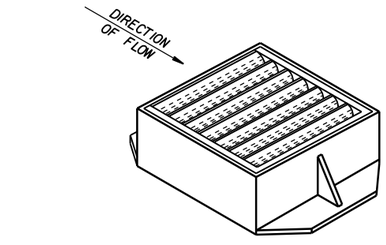
USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

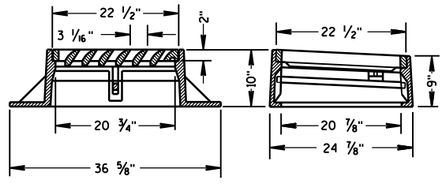
NOTE:
SPECIAL GRATE FOR THE
TYPE "H" COVER MAY ALSO BE
USED FOR THE TYPE "HM" COVER
NOTED AS TYPE HM-S ON DRAINAGE TABLE



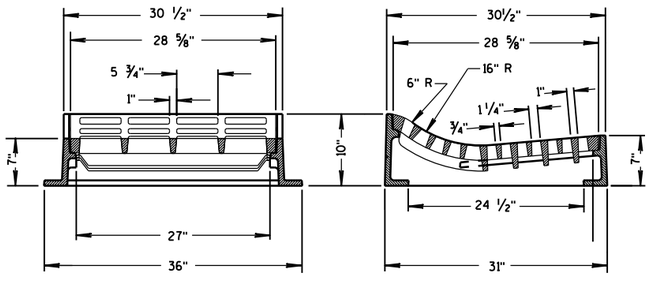


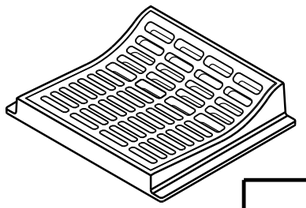
TYPE "S"





TYPE "V"





TYPE "T"

USE WITH TYPES R & T CONCRETE CURB & GUTTER, 36 INCH.

INLET COVERS
TYPE F, HM, HM-S, S, T, V, HM-GJ, & HM-GJ-S

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

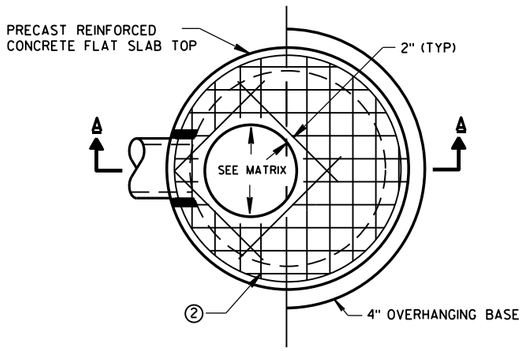
APPROVED
11/27/2013
DATE

/S/ Jerry H. Zogg
ROADWAY STANDARDS ENGINEER

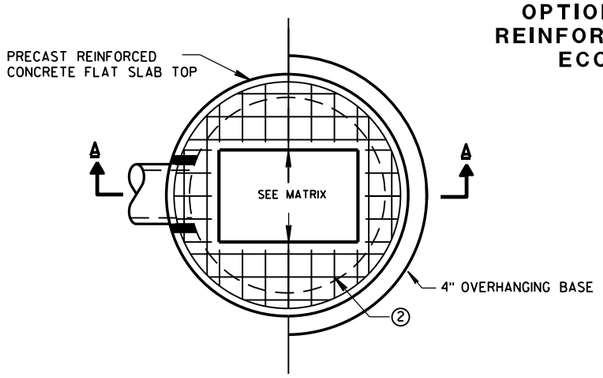
FHWA

6

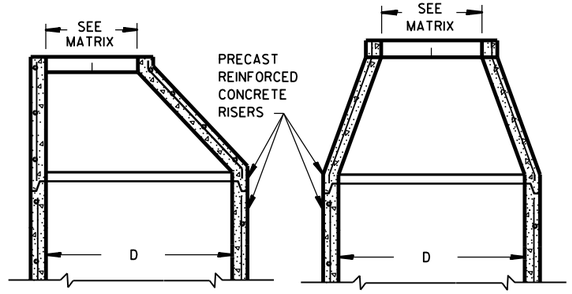
S.D.D. 8 A 5-19c



PLAN VIEW CIRCULAR OPENING

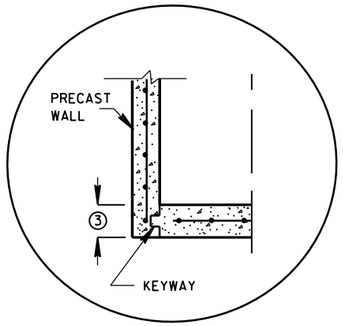


PLAN VIEW RECTANGULAR OPENING

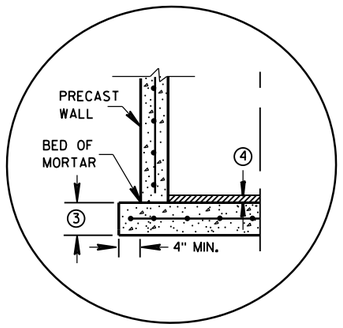


OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP

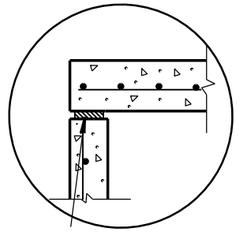


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION



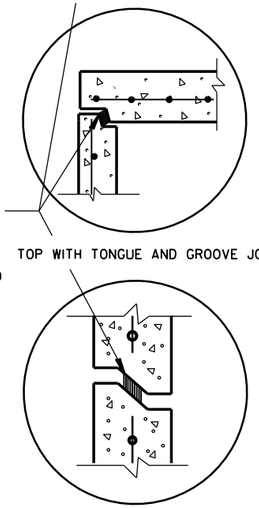
SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

DETAIL "A"



TOP WITH PLAIN END JOINT

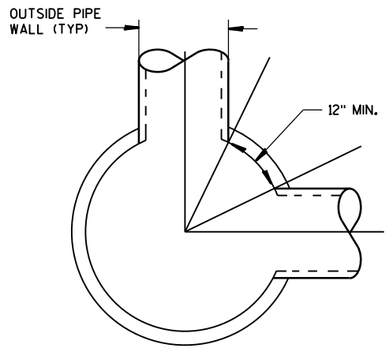
JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



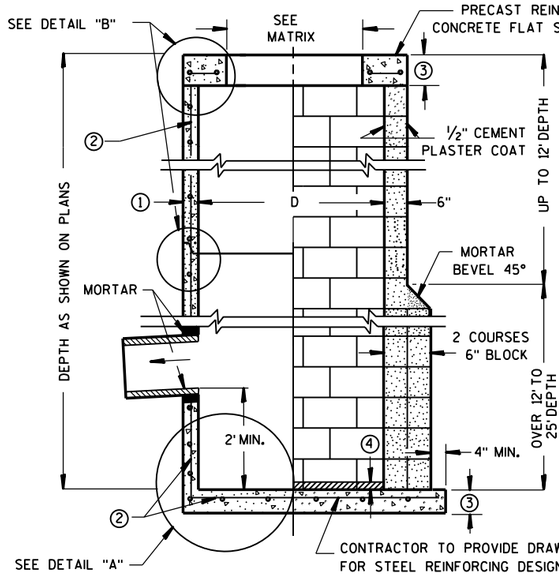
TOP WITH TONGUE AND GROOVE JOINT

RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"



DETAIL "C"



CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES

SECTION A-A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF GRANULAR BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES. THE TOPS SHALL BE INSTALLED ON A BED OF MORTAR.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT AND 7 INCHES FOR 6-FT DIAMETER PRECAST CATCH BASINS.

② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".

④ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2' SUMP MEASURED FROM TOP OF KEY.

CATCH BASIN COVER OPENING MATRIX

CATCH BASIN SIZE	INLET COVER TYPE	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
	OPENING SIZE (FT)											
3-FT	2X2	X	X					X		X		
	2 DIA.				X							X
4-FT-6-FT	2X2	X	X					X		X		
	2X2.5			X				X	X	X	X	
	2 DIA.				X							X
	2X3						X					
	2.5X3					X						

PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	30

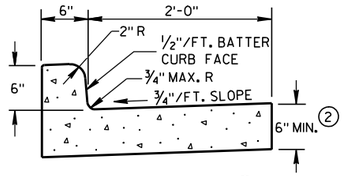
CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

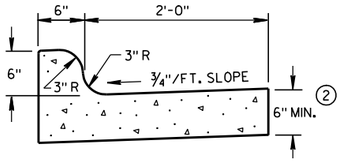
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/5/2012 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

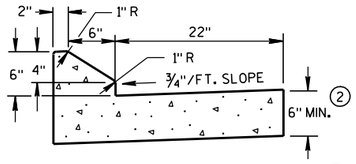
6
S.D.D. 8 A 8-1



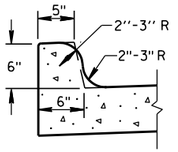
TYPES A & D (1)



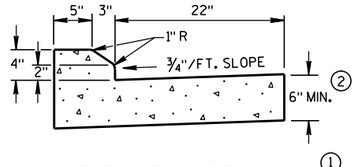
TYPES K & L (1)



6" SLOPED CURB TYPES G & J (1)

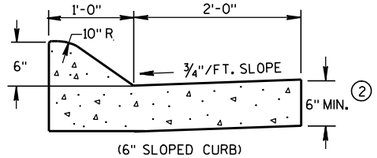


OPTIONAL CURB SHAPE FOR TYPES K & L (1)

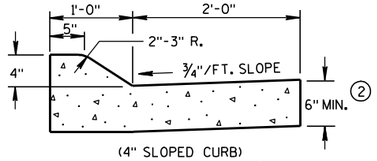


4" SLOPED CURB TYPES G & J (1)

CONCRETE CURB & GUTTER 30"

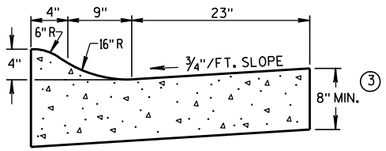


(6" SLOPED CURB)



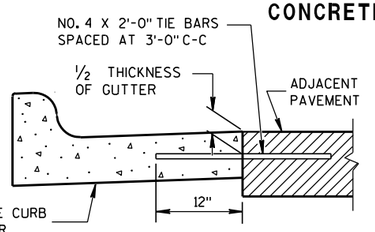
(4" SLOPED CURB)

TYPES A & D (1)

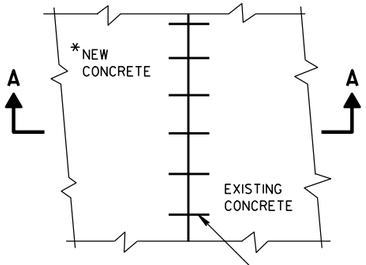


4" SLOPED CURB TYPES R & T (1) (4)

CONCRETE CURB & GUTTER 36"

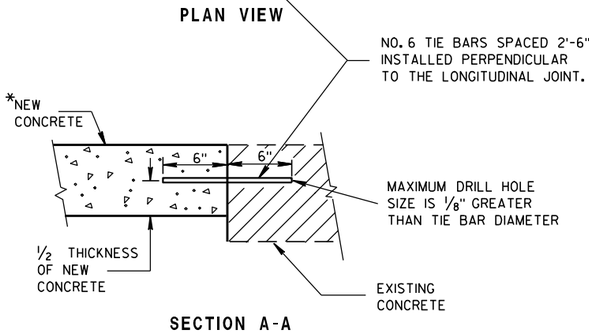


TYPICAL TIE BAR LOCATION (1)



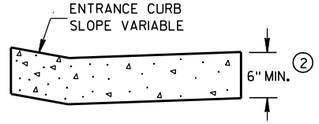
*NEW CURB & GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.

PLAN VIEW

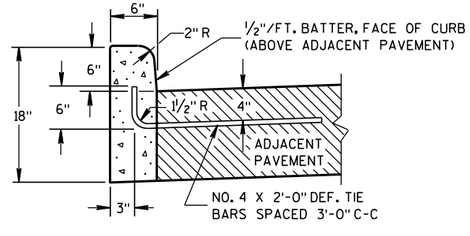


SECTION A-A

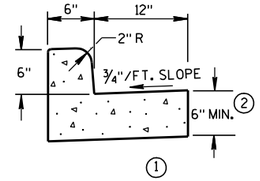
TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB
(WHEN DIRECTED BY THE ENGINEER)

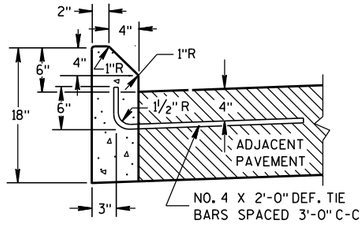


(1)
TYPES A & D



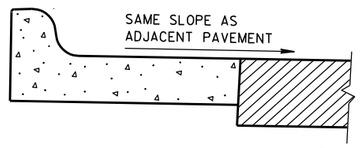
(1)
TYPES A & D

CONCRETE CURB & GUTTER 18"



(1)
TYPES G & J

CONCRETE CURB



REVERSE SLOPE GUTTER (5)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB & GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB & GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

WHERE THE TRANSVERSE JOINTS IN THE PAVEMENT ARE REQUIRED TO BE SEALED, THE JOINTS IN THE INTEGRAL CURB AND GUTTER SHALL BE SEALED TO THE FACE OF CURB WITH THE SAME TYPE OF SEALANT. THE COST OF FURNISHING AND INSTALLING THIS SEALANT SHALL BE INCIDENTAL TO THE ITEM CONCRETE CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

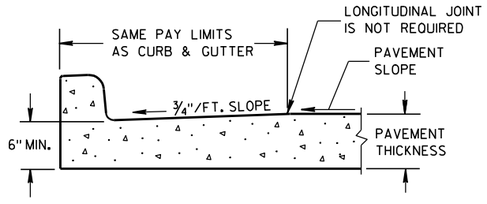
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPES A, G, K AND R.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

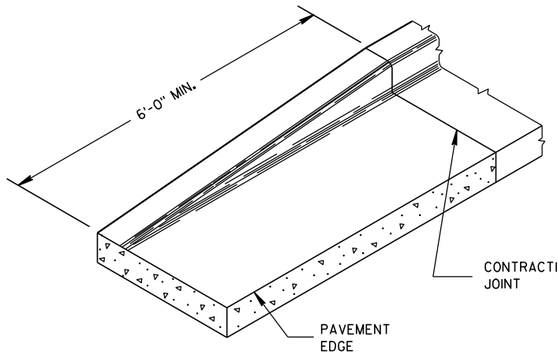
(3) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(4) THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(5) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB & GUTTER



END SECTION CURB & GUTTER

CONCRETE CURB, CONCRETE CURB & GUTTER AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
9/4/08 DATE
/S/ Jerry H. [illegible]
ROADWAY STANDARDS ENGINEER
FHWA

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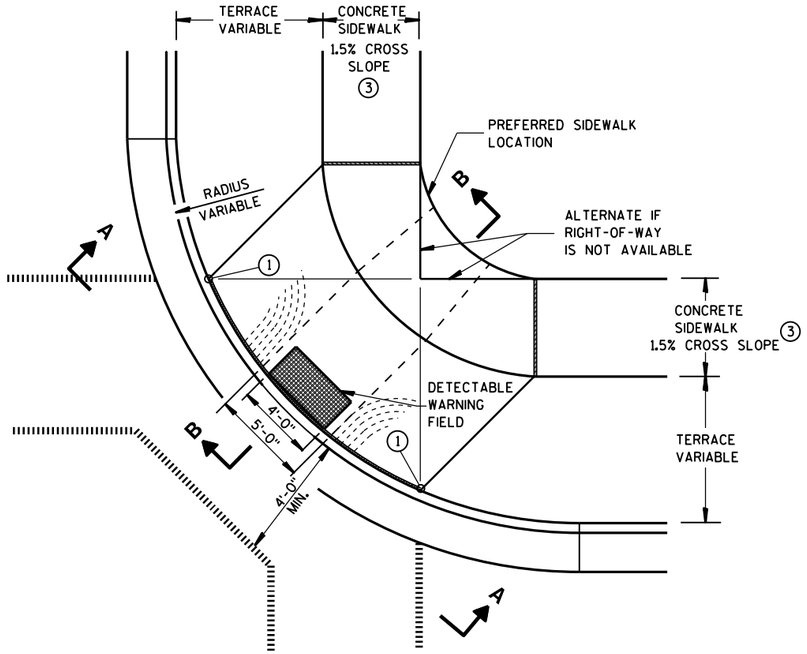
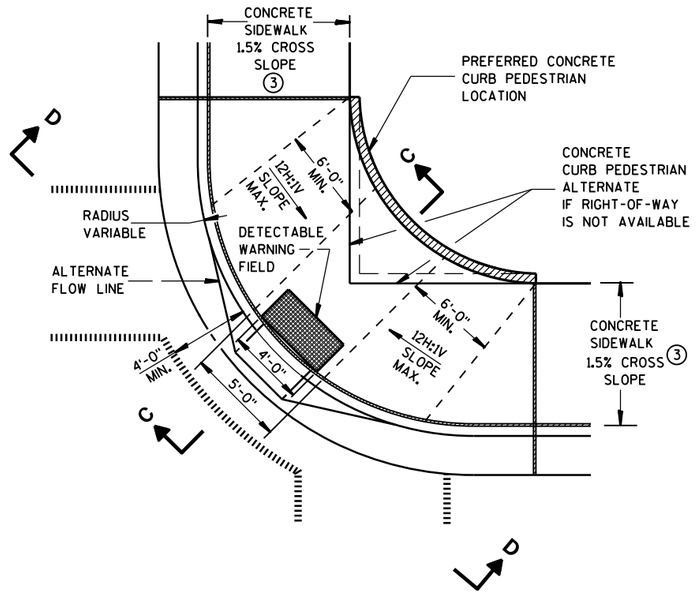
S.D.D. 8 D 1-17



PLAN VIEW

TYPE 1 RAMP

(CENTER OF CORNER RADIUS)



PLAN VIEW

TYPE 1-A RAMP

(NO TERRACE)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

RAMPS SHALL BE BUILT AT 12H:1V OR FLATTER. WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAL FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD".

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

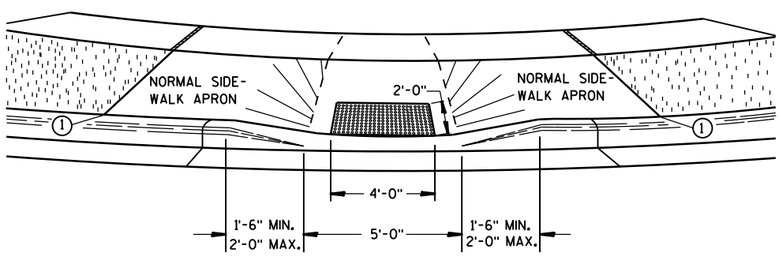
① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

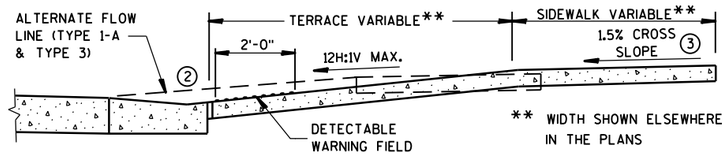
③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

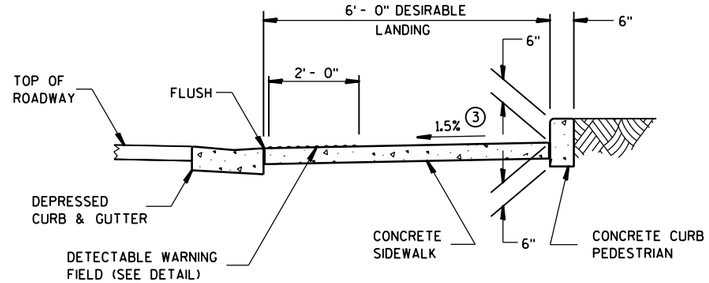
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



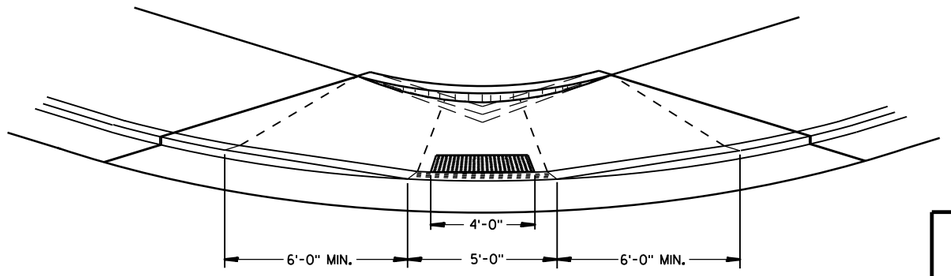
VIEW A-A



SECTION B-B



SECTION C-C



VIEW D-D

CURB RAMPS TYPES 1 AND 1-A

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR 66

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S.D.D. 8 D 5-15a

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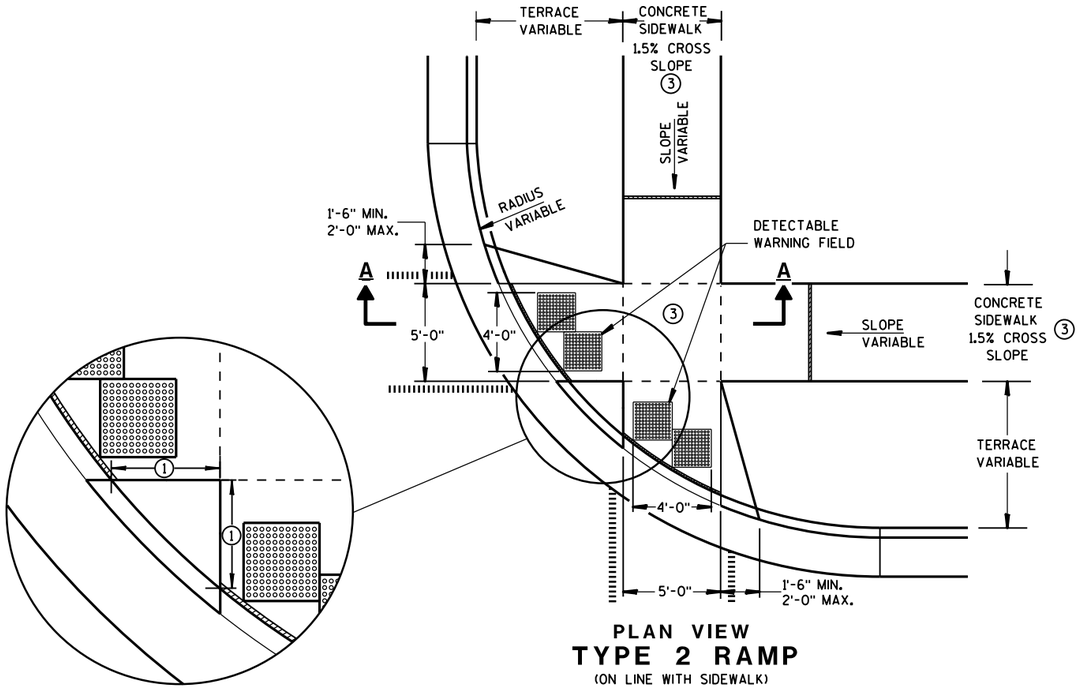
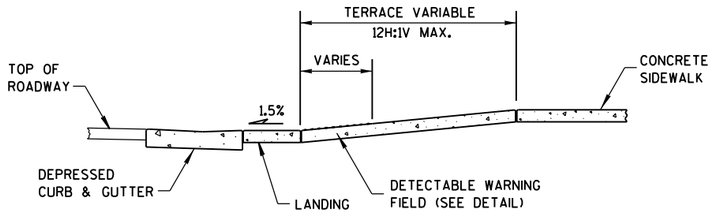
S.D.D. 8 D 5-15a



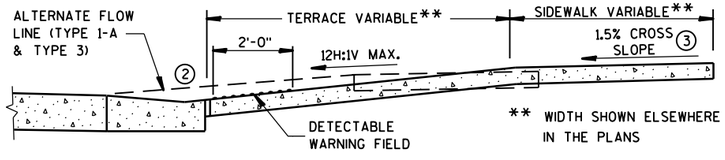
PLAN VIEW

TYPE 2 RAMP

(ON LINE WITH SIDEWALK)



SECTION A-A



SECTION B-B

GENERAL NOTES

USE THE TYPE 3 RAMP ONLY WHEN A TYPE 1 OR TYPE 2 CANNOT BE ACHIEVED BECAUSE OF FIELD CONDITIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

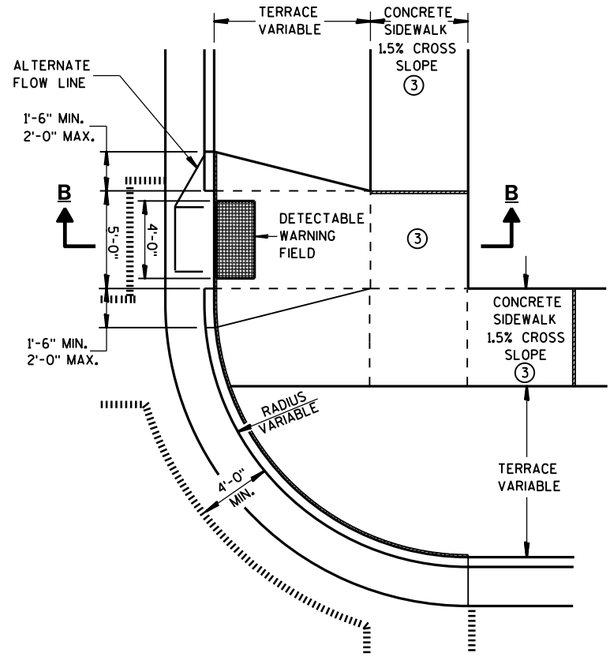
① WHEN THIS DISTANCE IS LESS THAN 6'-0" IT MAY BE DIFFICULT TO ACHIEVE A 12H:1V SLOPE, OR FLATTER, ON THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 12H:1V SLOPE, OR FLATTER, ON RAMP. 2" MINIMUM CURB HEIGHT.

② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. PROVIDE DRAINAGE AWAY FROM CURB RAMP AT GUTTER FLAG INTERFACE.

③ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)
- ALTERNATIVE LAYOUT



PLAN VIEW

TYPE 3 RAMP

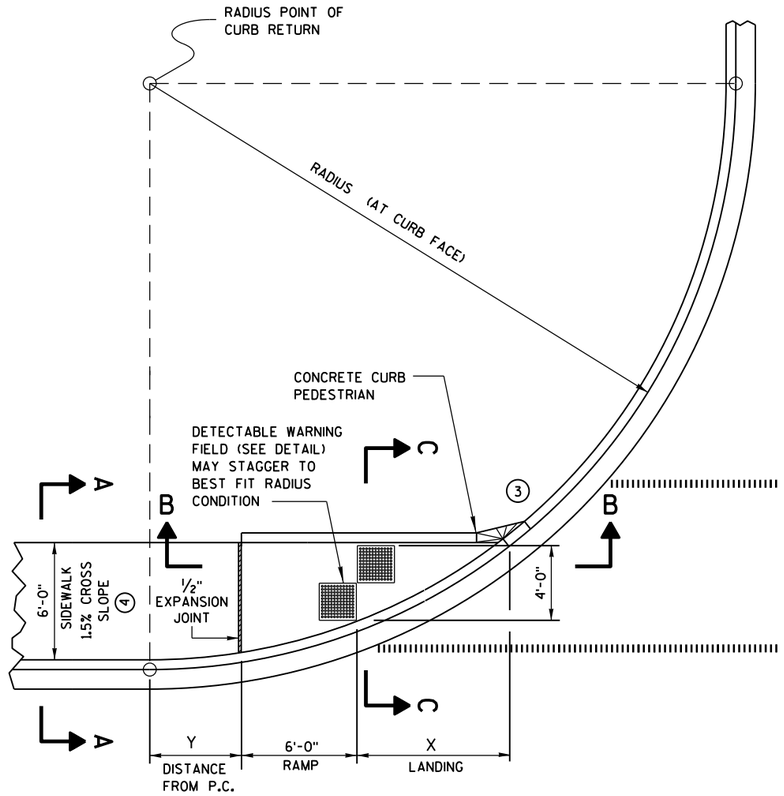
(OUTSIDE OF CROSSWALK AREA)

CURB RAMPS TYPES 2 AND 3

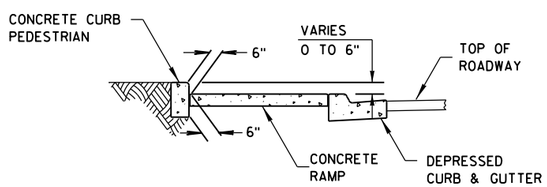
STATE OF WISCONSIN
DEPARTMENT OF TRANSPO

6

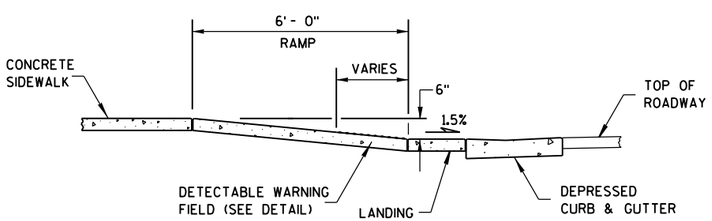
S.D.D. 8 D 5-15b



CURB RAMP TYPE 4A
PLAN VIEW



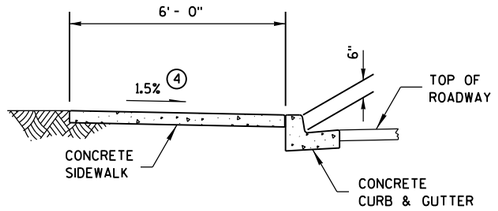
SECTION C-C FOR TYPE 4A



SECTION B-B FOR TYPE 4A

RADIUS (AT CURB FACE)	X	Y
20 FEET	6'-1¾"	2'-7¼"
30 FEET	7'-11¾"	4'-8¼"
40 FEET	9'-5¼"	6'-5"
50 FEET	10'-8¾"	7'-11¼"
60 FEET	11'-10¼"	9'-3½"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A-A FOR TYPE 4A

GENERAL NOTES

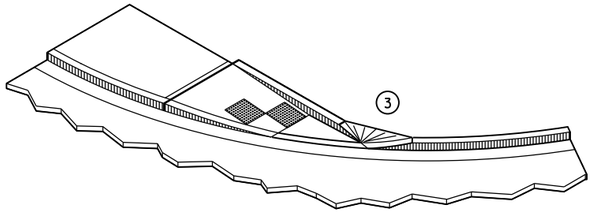
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

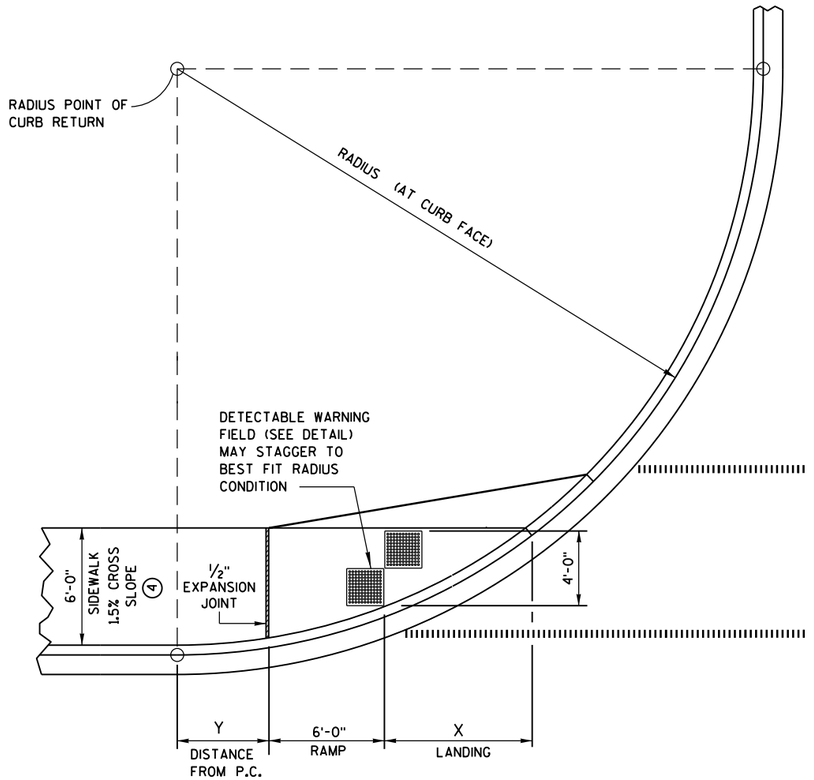
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

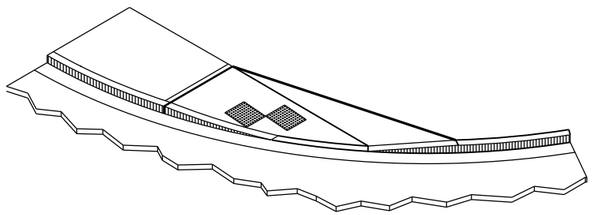
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



ISOMETRIC VIEW FOR TYPE 4A



CURB RAMP TYPE 4A1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4A1

LEGEND

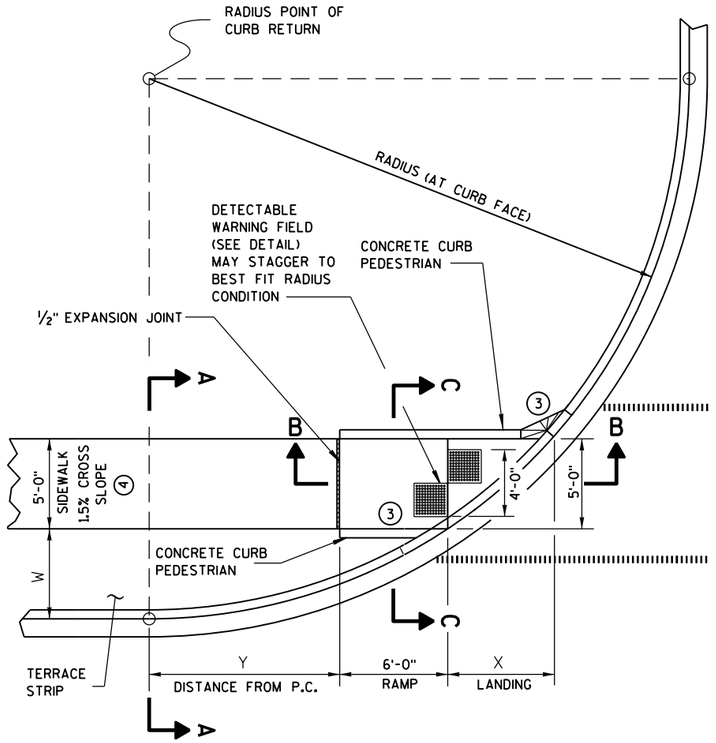
- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPES 4A AND 4A1

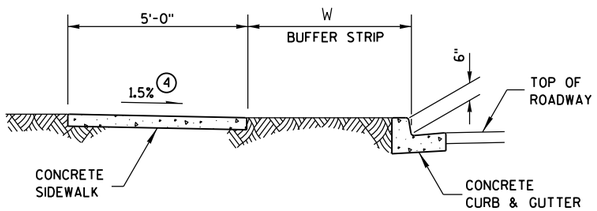
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

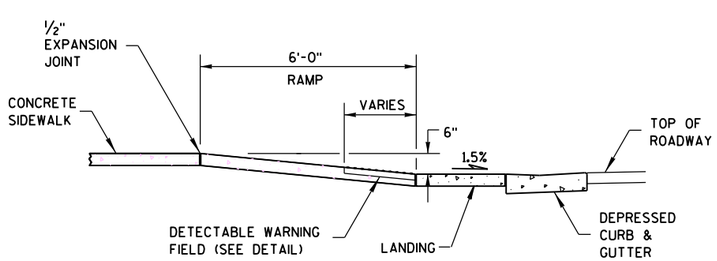
S.D.D. 8 D 5-15c



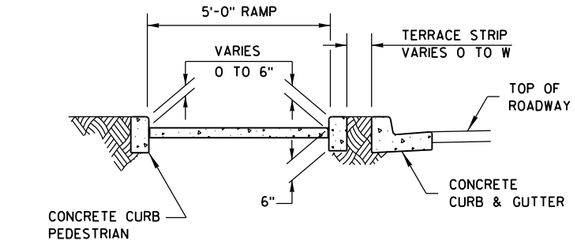
CURB RAMP TYPE 4B
PLAN VIEW



SECTION A-A FOR TYPE 4B



SECTION B-B FOR TYPE 4B



SECTION C-C FOR TYPE 4B

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

RADIUS (AT CURB FACE)	W = 3'-0"		W = 4'-0"		W = 5'-0"		W = 6'-0"		W = 7'-0"	
	X	Y	X	Y	X	Y	X	Y	X	Y
20 FEET	5'-5½"	4'-6½"	4'-8½"	6'-0"	4'-1"	7'-2¾"	3'-7"	8'-3½"	3'-1½"	9'-2½"
30 FEET	7'-3¾"	7'-1"	6'-5½"	8'-11½"	5'-9¼"	10'-7"	5'-2½"	12'-0"	4'-8¾"	13'-3¼"
40 FEET	8'-9½"	9'-2½"	7'-10"	11'-5¼"	7'-1"	13'-4½"	6'-5¾"	15'-¾"	5'-11½"	16'-7¼"
50 FEET	10'-¾"	11'-¾"	9'-¼"	13'-7¼"	8'-2½"	15'-9½"	7'-6½"	17'-9"	6'-11¾"	19'-6¼"
60 FEET	11'-2½"	12'-8¾"	10'-¾"	15'-6½"	9'-2¼"	17'-11¾"	8'-5¾"	20'-1¾"	7'-10½"	22'-1½"

INTERMEDIATE RADII CAN BE INTERPOLATED

GENERAL NOTES

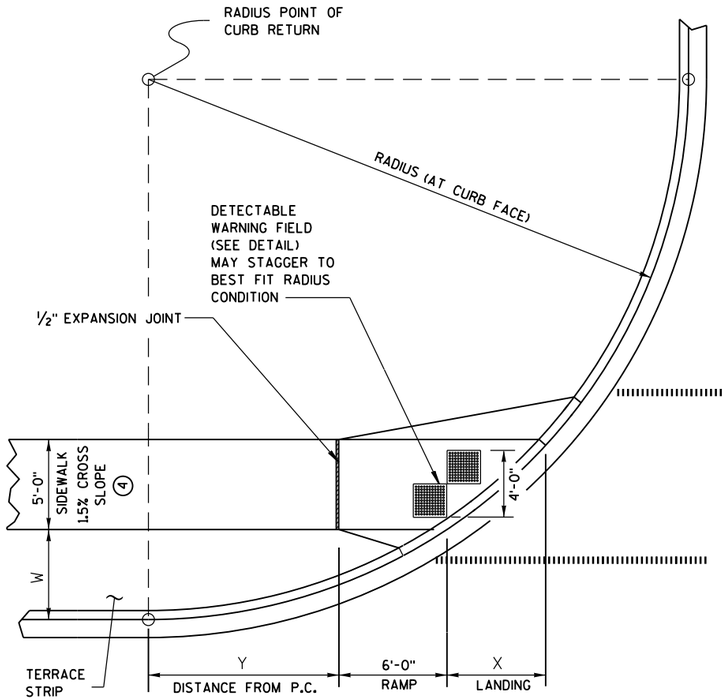
AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1.

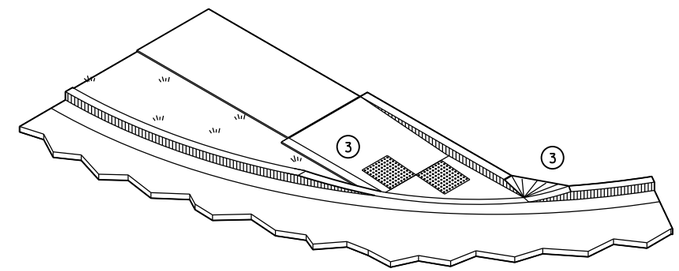
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

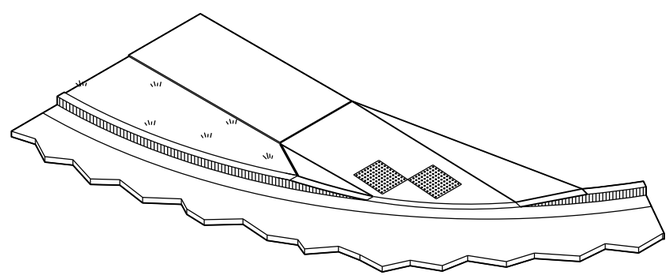
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



CURB RAMP TYPE 4B1
PLAN VIEW



ISOMETRIC VIEW FOR TYPE 4B



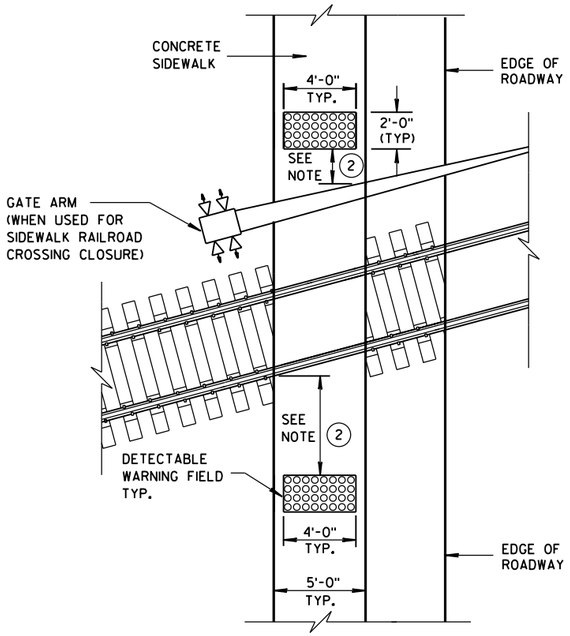
ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS TYPE 4B AND 4B1

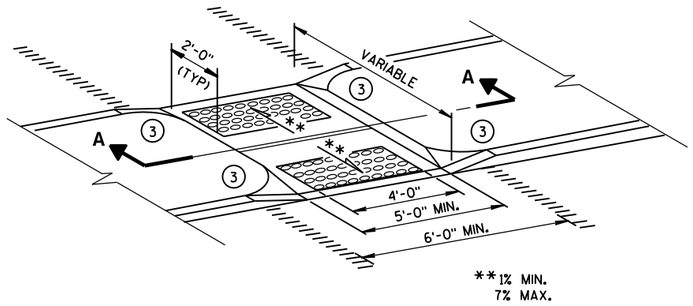
STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 8 D 5-15d



TYPE 8
DETECTABLE WARNINGS AT RAILROAD CROSSING



MEDIAN ISLAND
NON-ELEVATED CROSSING
TYPE 5

GENERAL NOTES

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

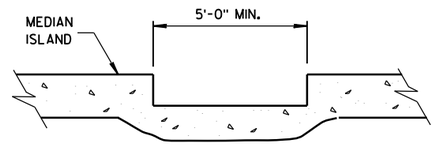
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(1) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

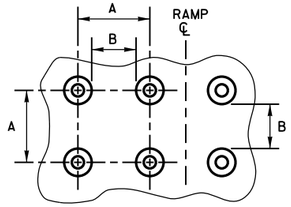
(2) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ± 0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

(3) INSTALL TRANSITION NOSE. (INCIDENTAL TO OTHER PAY ITEMS.) DO NOT MARK TRANSITION NOSE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.



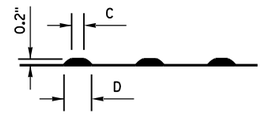
SECTION A-A



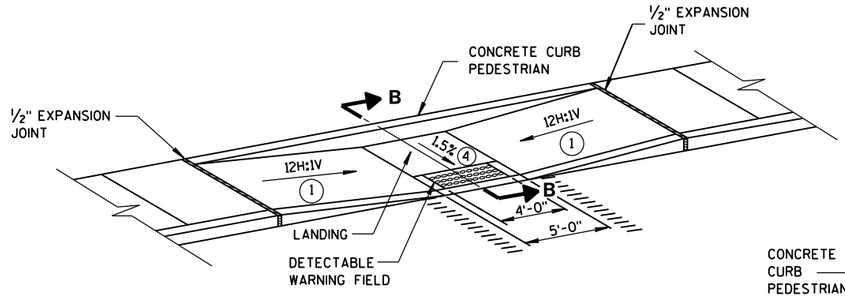
PLAN VIEW

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

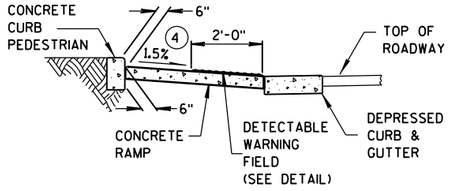
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



ELEVATION VIEW

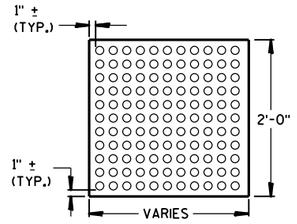


MID-BLOCK CROSSING
TYPE 7A



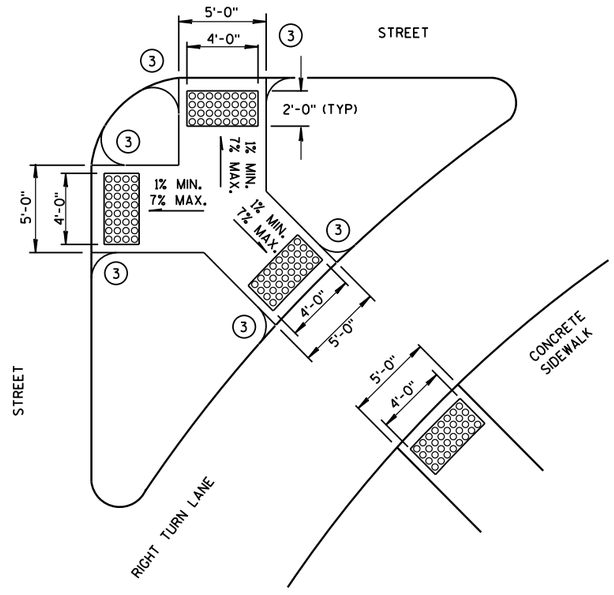
SECTION B-B

TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL

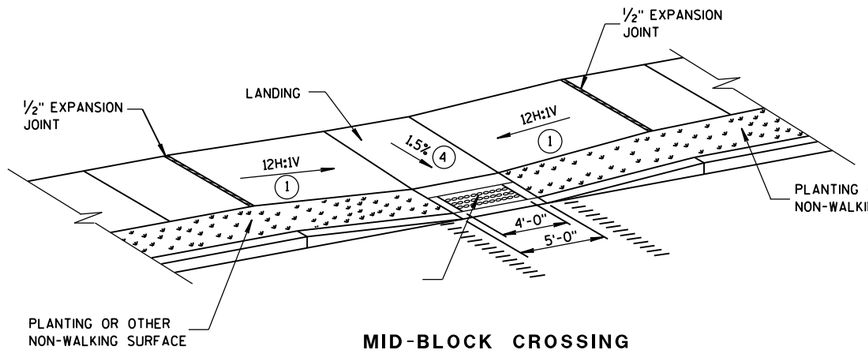


PLAN VIEW

DETECTABLE WARNING FIELD (TYPICAL)



TYPE 6
DETECTABLE WARNING AT ISLANDS



MID-BLOCK CROSSING
TYPE 7B

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

LEGEND

- ½" EXPANSION JOINT-SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

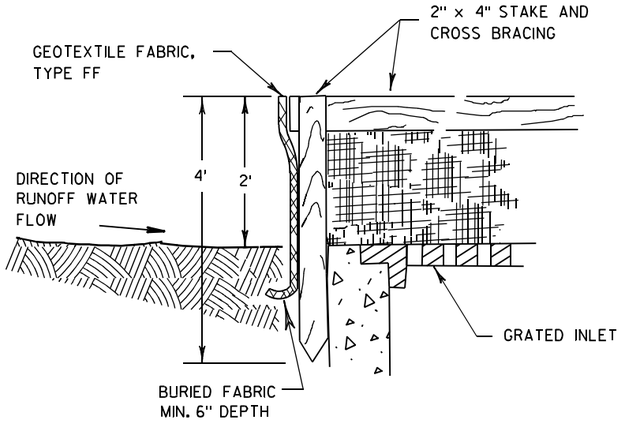
CURB RAMPS
TYPES 5, 6, 7A, 7B & 8

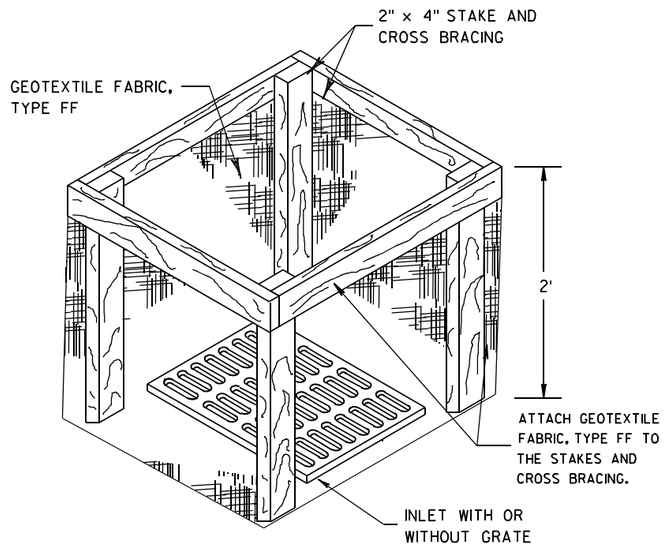
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
2-6-2013 DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

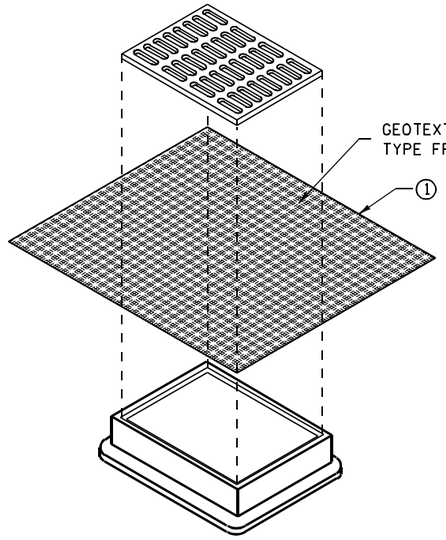
6 S.D.D. 8 D 5-15e

6 S.D.D. 8 D 5-15e



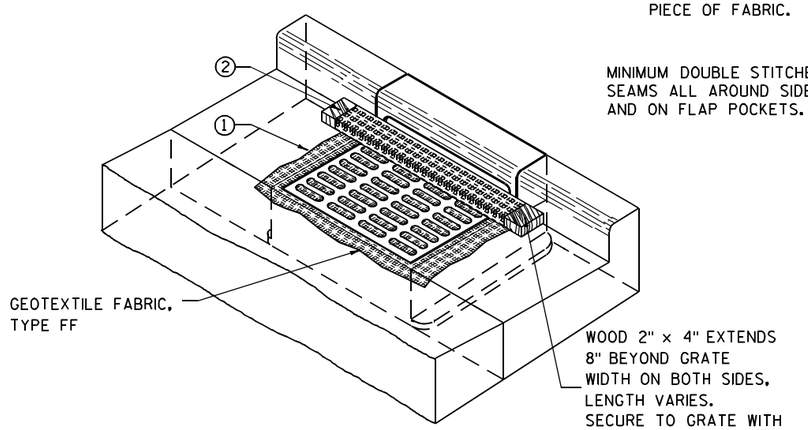


INLET PROTECTION, TYPE A

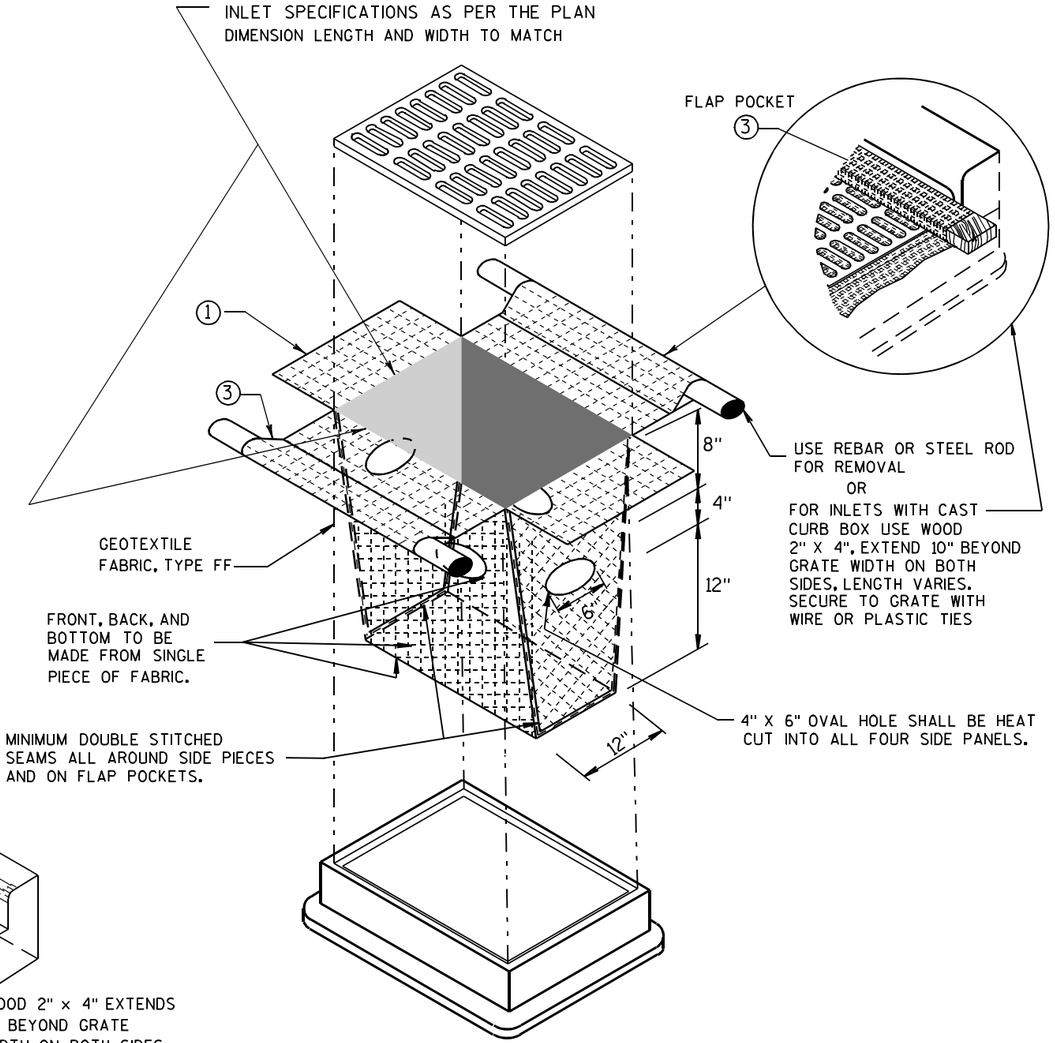


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

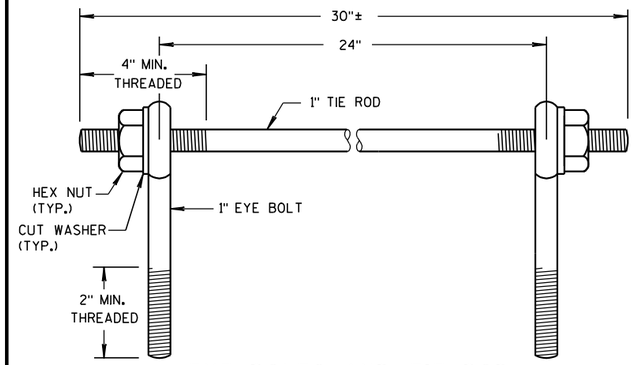
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

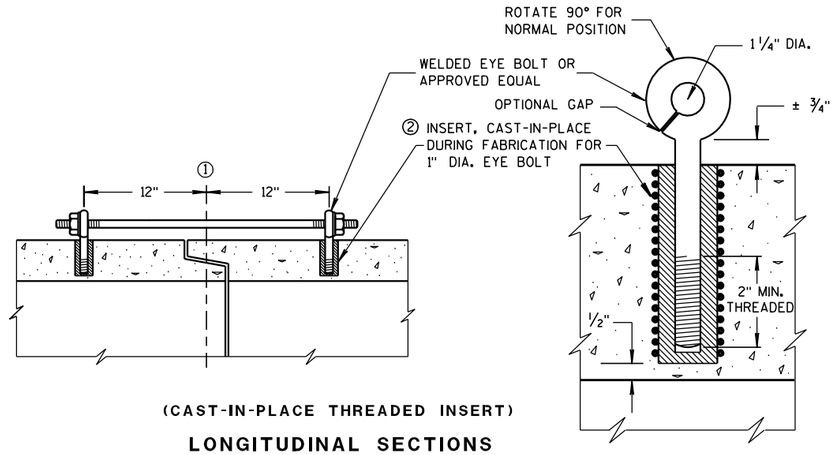
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6
S.D.D. 8 E 10-2



EYE BOLTS AND TIE ROD



(CAST-IN-PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

① ℄ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

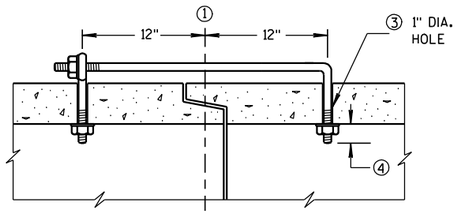
② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ℄ OF TONGUE AND GROOVE.

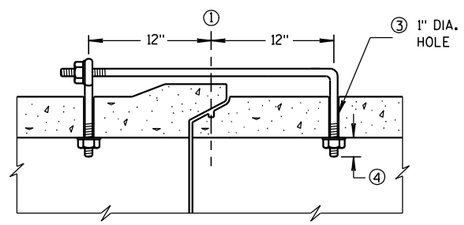
④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.

⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.



(TONGUE & GROOVE PIPE)

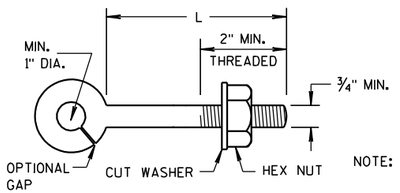


(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

EYE BOLT DIMENSION TABLE

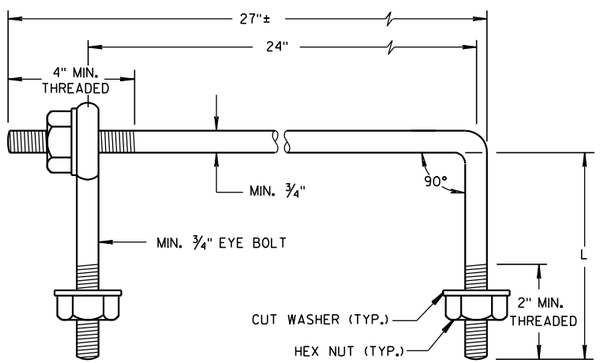
PIPE SIZE	L = LENGTH	
	TONGUE & GROOVE PIPE	MODIFIED BELL PIPE
18" TO 24"	4 1/2"	6 1/4"
30"	5"	7"
36"	5 1/2"	7"
42"	6"	
48"	6 1/2"	
60"	7 1/2"	
66"	8"	



NOTE:

EYE BOLT

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD

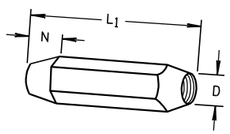
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12-60	5/8	5/8	5	1/2
66-84	3/4	3/4	5	1/2
90-108	1	1	7	1 1/16

DIMENSIONS SHOWN ARE IN INCHES



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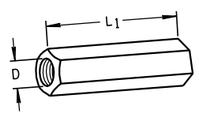
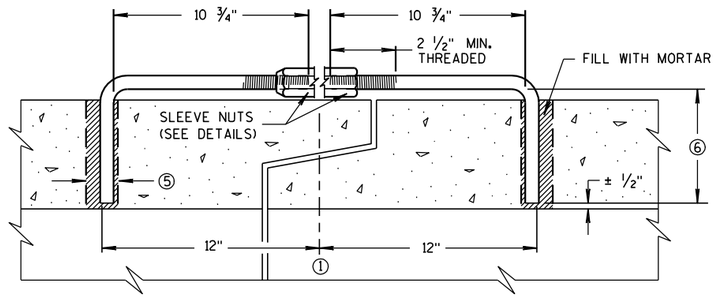


PLAIN

RIGHT AND LEFT THREADS

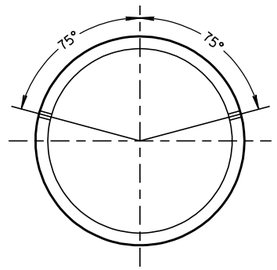
SLEEVE NUTS



LONGITUDINAL SECTION

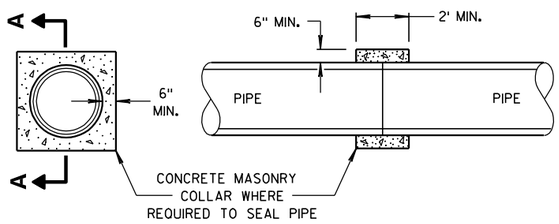
(JOINT TIES FOR 12" TO 108" DIA. CONCRETE PIPE)

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A-A

CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/5/2012 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
72
FHWA

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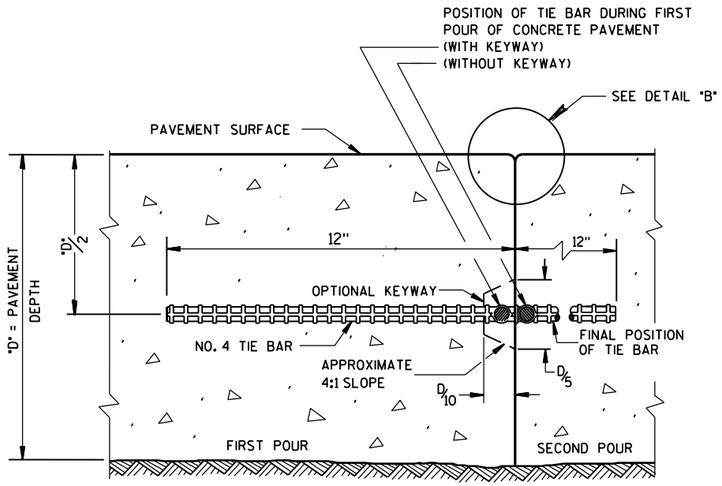
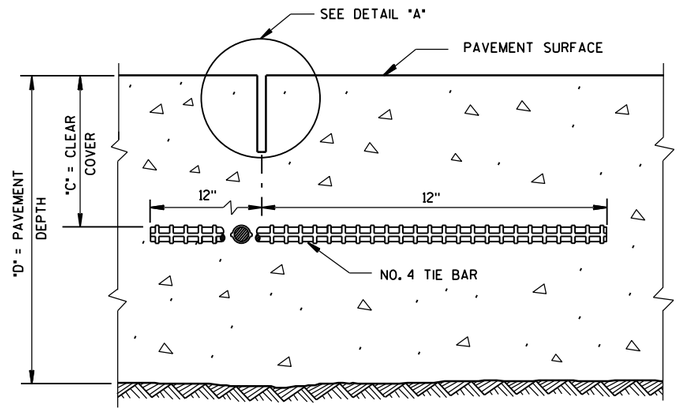


CONSTRUCTION JOINT



SAWED JOINT

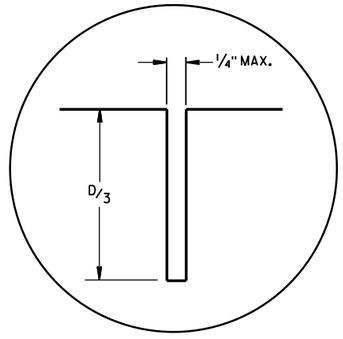
GENERAL NOTES

DO NOT SEAL OR FILL LONGITUDINAL JOINTS.

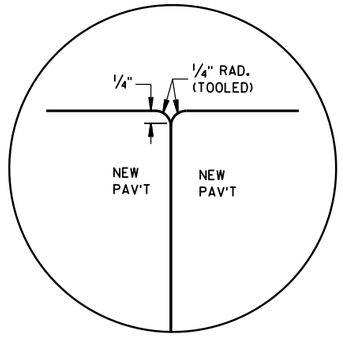
CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.

CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

(1) ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.



DETAIL "A"



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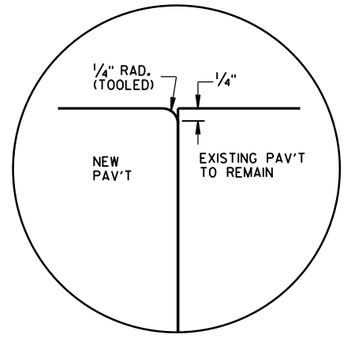

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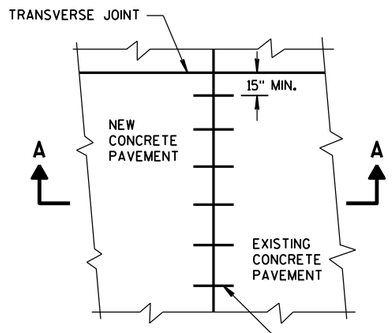
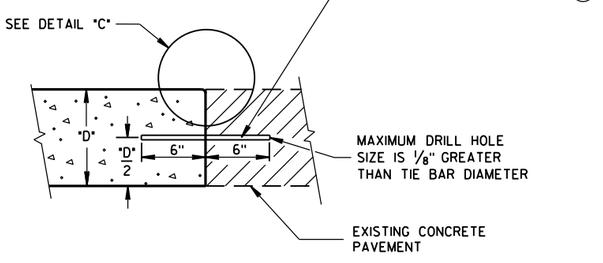


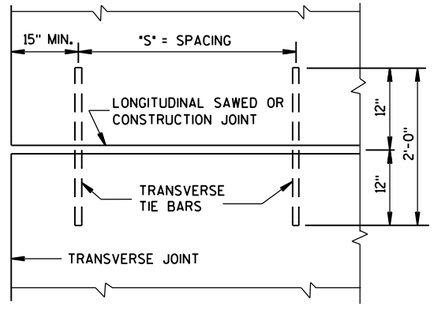
PLAN VIEW



SECTION A-A
LONGITUDINAL CONSTRUCTION JOINT TIE BARS ANCHORED INTO EXISTING PAVEMENT

TIE BAR TABLE

PAVEMENT DEPTH "D"	CLEAR COVER "C"	MAXIMUM TIE BAR SPACING "S"	
		PAVEMENT WIDTH	
		24' OR 26'	≥30'
6, 6 1/2"	3"±1/2"	48"	42"
7, 7 1/2"	3 1/4"±1"	45"	36"
8, 8 1/2"	3 3/4"±1"	39"	30"
9, 9 1/2"	4 1/4"±1"	33"	27"
10, 10 1/2"	4 3/4"±1"	30"	24"
11, 11 1/2"	5 1/4"±1"	27"	21"
12"	5 3/4"±1"	24"	21"



PLAN VIEW
SHOWING LOCATION OF TIE BARS

CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

9/2014 DATE

/S/ Deb Bischoff
PAVEMENT POLICY & DE ...NEER

FHWA

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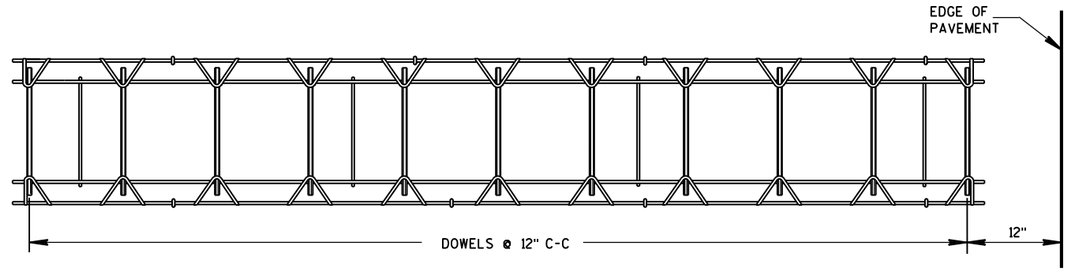
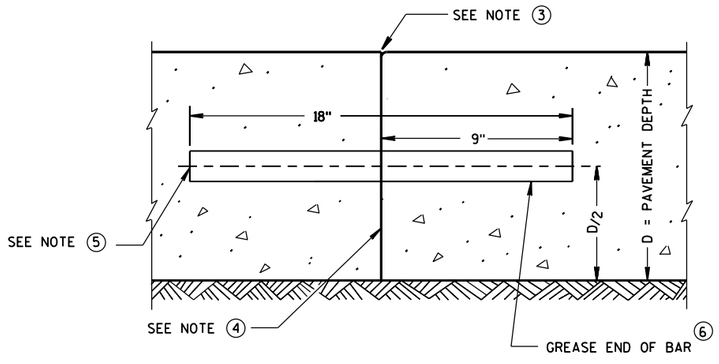


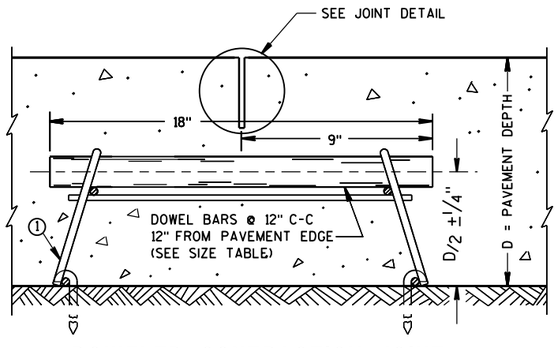
PLAN VIEW

SIDE VIEW

CONTRACTION JOINT DOWEL ASSEMBLY ①



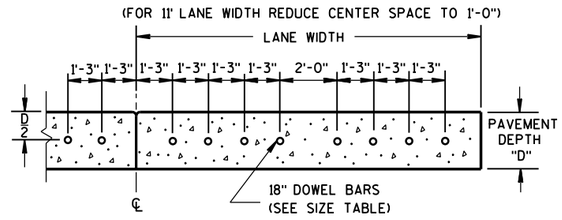
TRANSVERSE CONSTRUCTION JOINT



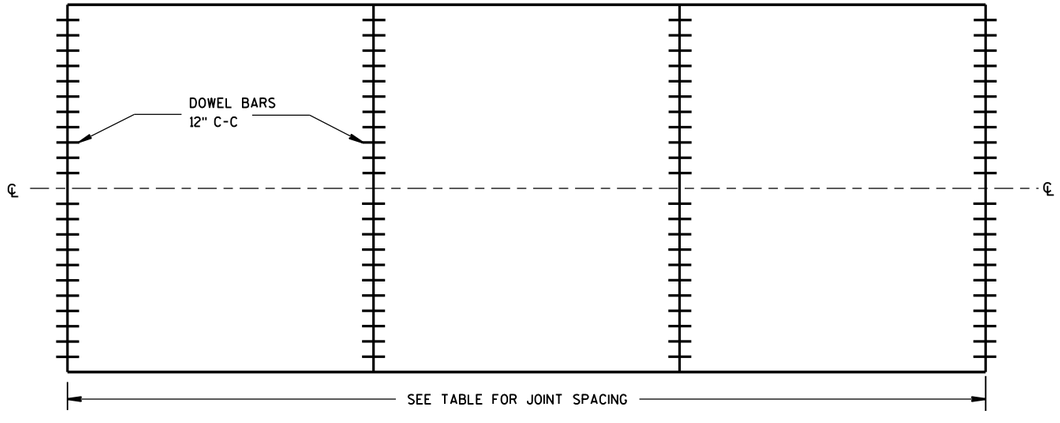
DOWELED CONTRACTION JOINT

PAVEMENT DEPTH, DOWEL BAR SIZE AND JOINT SPACING TABLE

PAVEMENT DEPTH (D)	DOWEL BAR DIAMETER	CONTRACTION JOINT SPACING
5 ½", 6", 6 ½"	NONE	12'
7", 7 ½"	1"	14'
8", 8 ½"	1 ¼"	15'
9", 9 ½"	1 ¼"	15'
10" & ABOVE	1 ½"	15'



DRILLED DOWEL BAR CONSTRUCTION JOINT ⑦



CONTRACTION JOINT LOCATIONS

GENERAL NOTES

CONTRACTION JOINTS

CONSTRUCT TRANSVERSE CONTRACTION JOINTS NORMAL TO THE CENTERLINE. SHOW THE LOCATION OF CONTRACTION JOINTS THROUGH INTERSECTIONS ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

DO NOT SEAL OR FILL CONTRACTION JOINTS.

INSTALL DOWEL BARS PARALLEL TO THE PAVEMENT CENTERLINE AND PAVEMENT SURFACE.

FOR PAVEMENT SLABS OF VARYING WIDTHS, LOCATE THE OUTER MOST DOWEL BAR SO THAT THE CENTER OF THE BAR IS A MINIMUM OF 6 INCHES AND A MAXIMUM OF 18 INCHES FROM THE LONGITUDINAL JOINT AND THE FREE EDGE OF PAVEMENT.

CONSTRUCTION JOINTS

LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.

① **OBTAIN THE ENGINEER'S APPROVAL FOR THE USE OF ALTERNATIVE DESIGNS OF THE DOWEL ASSEMBLY. USE MECHANICAL DOWEL BAR INSERTERS OR DOWEL ASSEMBLIES WHEN CONSTRUCTING CONTRACTION JOINTS.**

② SECURE BASKETS WITH ANCHORS TO HOLD DOWEL BARS IN THE CORRECT POSITION AND ALIGNMENT. TYPE, LOCATION, NUMBER AND LENGTH OF ANCHORS ARE DEPENDENT UPON FIELD CONDITIONS.

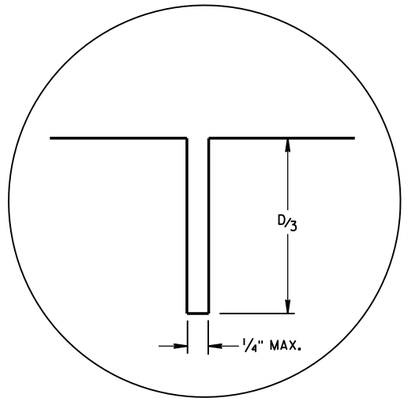
③ FORM OR SAW CONSTRUCTION JOINTS. PROVIDE A ¼-INCH RADIUS AT FORMED JOINTS.

④ PROVIDE A SMOOTH VERTICAL FACE FOR THE ENTIRE DEPTH OF THE PAVEMENT WHEN FORMING CONSTRUCTION JOINTS.

⑤ INSTALL DOWEL BARS AT CONSTRUCTION JOINTS BY FORMING OR DRILLING. INSTALL FORMED DOWEL BARS 12 INCHES C-C AND 12 INCHES FROM PAVEMENT EDGE. REMOVE EXCESS CONCRETE FROM THE FREE END OF THE DOWEL BAR IF DOWEL BARS ARE FORMED THROUGH A HEADER BOARD. INSTALL DRILLED DOWEL BARS ACCORDING TO *DRILLED DOWEL BAR CONSTRUCTION JOINT* DETAIL.

⑥ APPLY A THIN UNIFORM COATING OF SURFACE TREATMENT TO THE FREE END OF DOWEL BARS TO PREVENT BONDING.

⑦ ANCHOR DOWEL BARS INTO DRILLED HOLES WITH AN EPOXY. MAXIMUM DRILLED HOLE SIZE IS ⅛-INCH GREATER THAN DOWEL BAR DIAMETER, 9 INCHES IN LENGTH.



JOINT DETAIL

URBAN DOWELED CONCRETE PAVEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 DATE
/S/ Deb Bischoff
PAVEMENT POLICY & DE ENGINEER
FHWA

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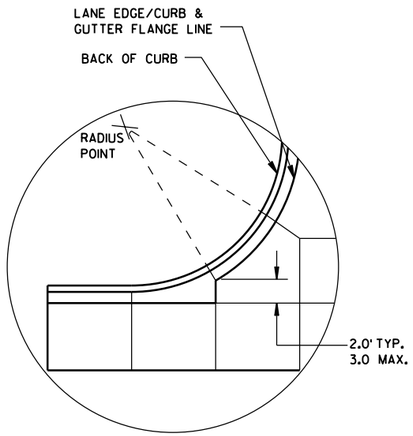
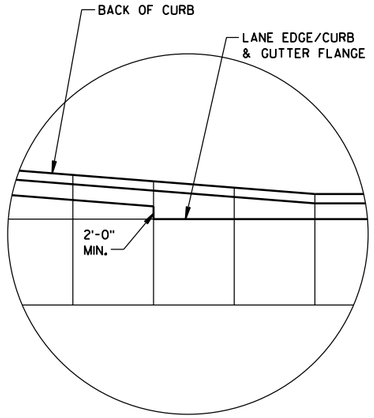


DETAIL "A"



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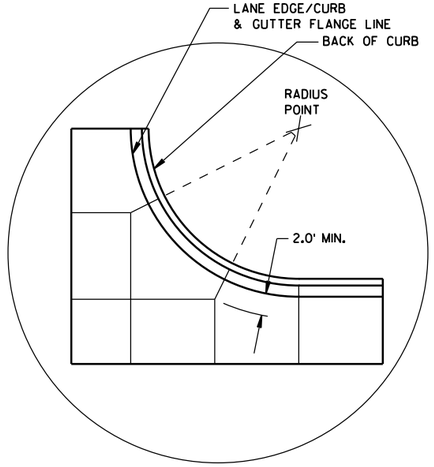

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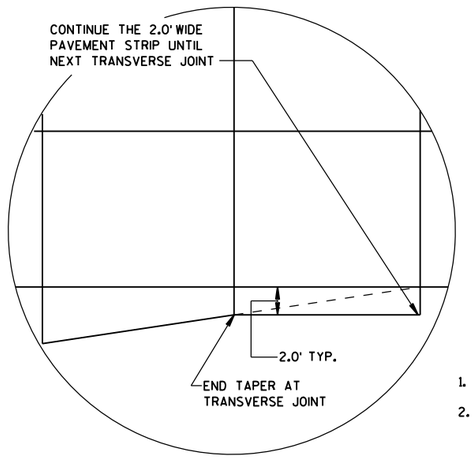


DETAIL "D"

GENERAL NOTES

THE PRIMARY ROADWAY CONTROLS THE TRANSVERSE JOINT PATTERN.

ALIGN NEW JOINTS WITH EXISTING JOINTS OR CRACKS.

CONSTRUCT TRANSVERSE JOINTS PERPENDICULAR TO THE ROADWAY.

ADJUST TRANSVERSE JOINTS TO ALIGN WITH UTILITY FIXTURES (E.G. MANHOLES AND INLETS) IN THE PAVEMENT STRUCTURE WHEN POSSIBLE. WATER VALVES DO NOT REQUIRE JOINT ADJUSTMENT.

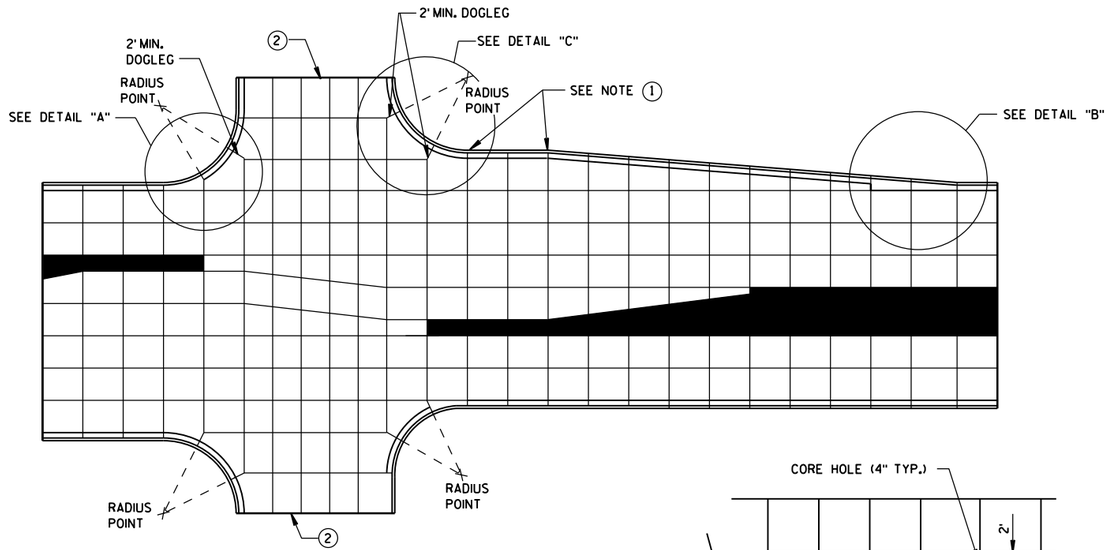
AVOID SLABS LESS THAN 2 FEET WIDE OR GREATER THAN 15 FEET WIDE.

SEE TABLE FOR TRANSVERSE JOINT SPACING. JOINT SPACING SPECIFIED IS MAXIMUM AND ACTUAL SPACING CAN BE ADJUSTED TO ACCOMMODATE INTERSECTIONS.

AVOID ANGLES LESS THAN 60° BY DOGLEGGING JOINTS THROUGH CURVE RADIUS POINTS. USE 90° ANGLES WHEN POSSIBLE.

CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

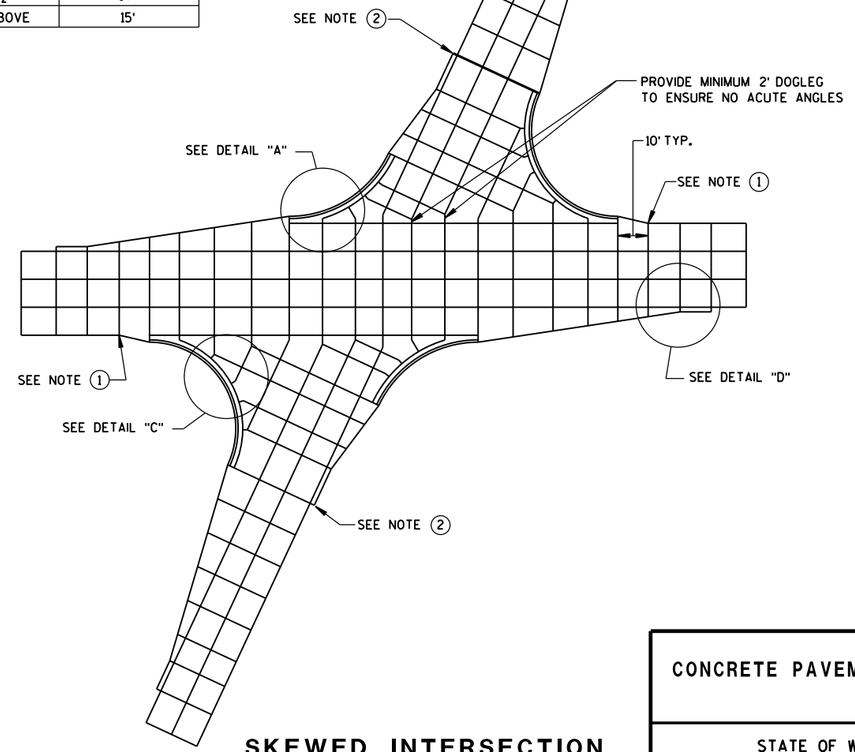
1. PROVIDE TRANSVERSE JOINTS AT ALL PAVEMENT WIDTH CHANGES.
2. CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF AN INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH. ALIGN EXPANSION JOINT WITH EDGE OF RADIUS.
3. THE ENGINEER MAY APPROVE SLIGHT VARIATIONS FROM THESE JOINTING DETAILS.



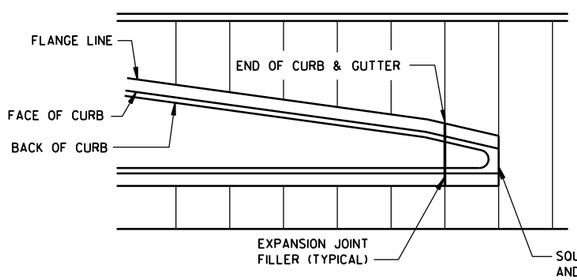
STANDARD INTERSECTION

PAVEMENT DEPTH AND JOINT SPACING TABLE

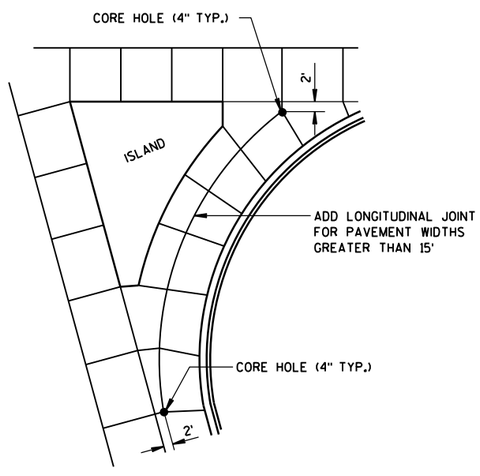
PAVEMENT DEPTH (D)	CONTRACTION JOINT SPACING
6", 6½"	12'
7", 7½"	14'
8" & ABOVE	15'



SKEWED INTERSECTION



APPROACH TO MEDIAN



LARGE RIGHT TURN

CONCRETE PAVEMENT JOINTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

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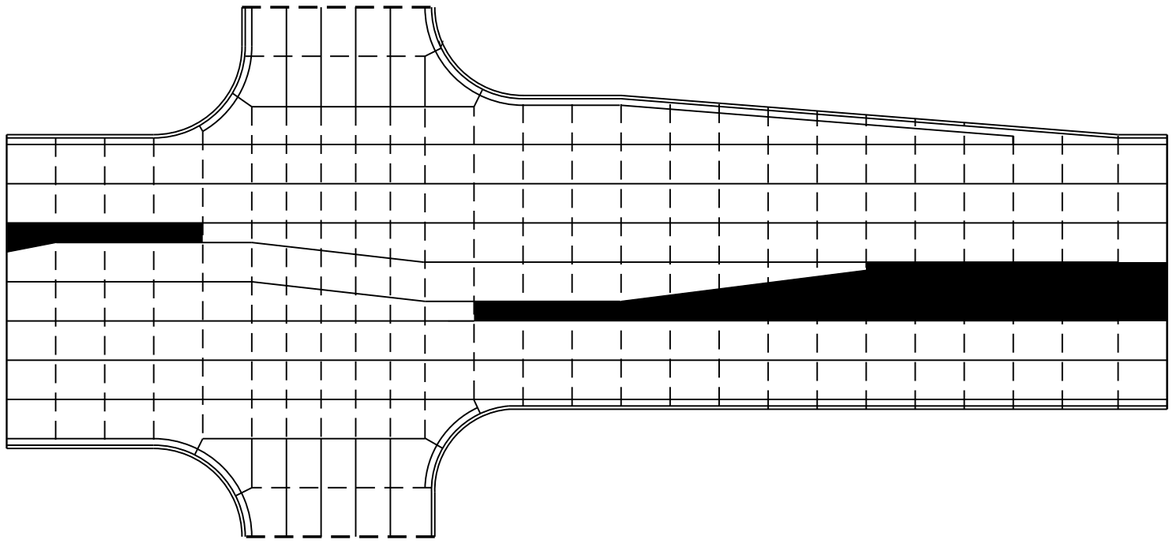
S.D.D. 13 C 18-2a

LEGEND

POTENTIAL DOWELED EXPANSION JOINT

DOWELED JOINT

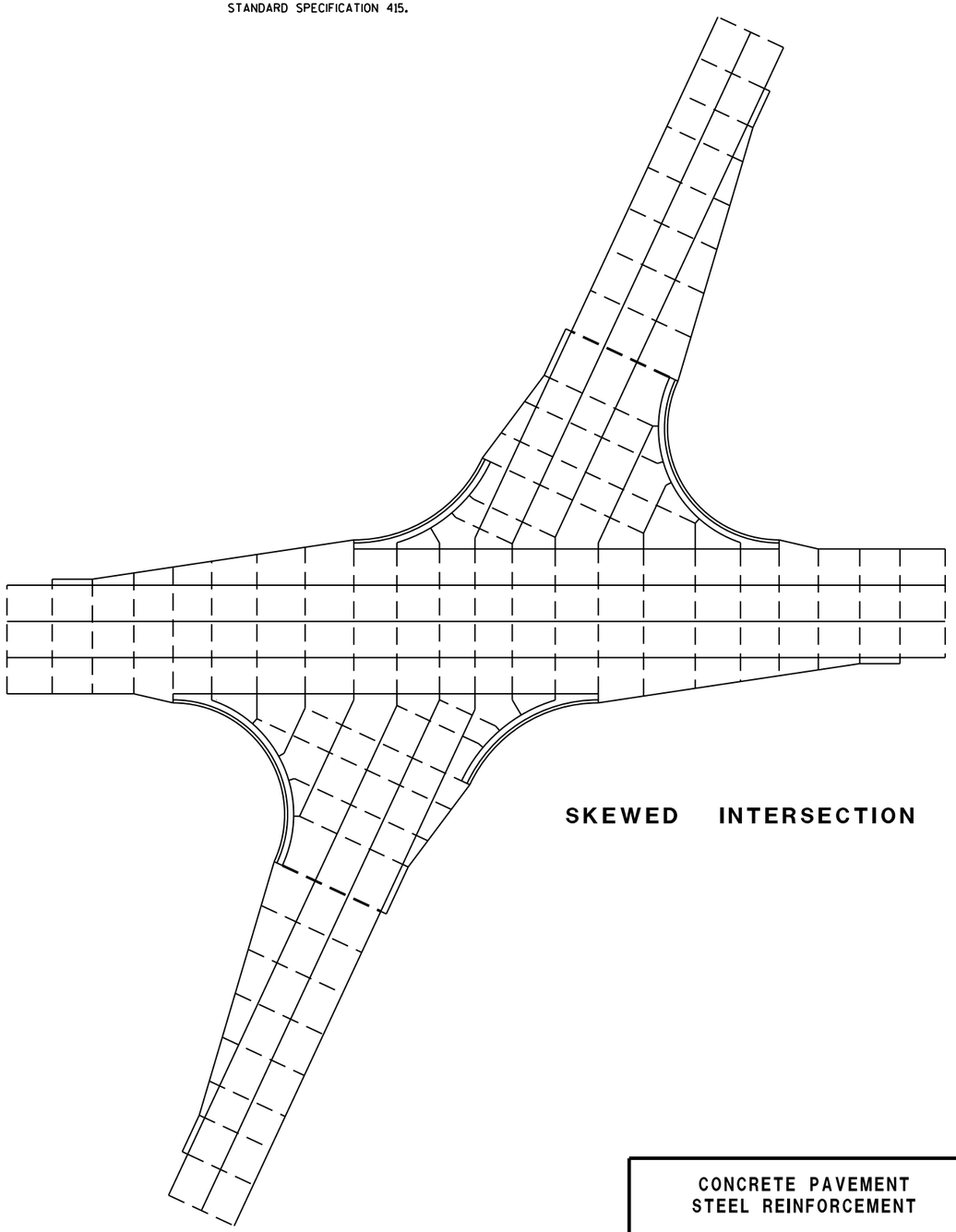
TIED JOINT



STANDARD INTERSECTION

GENERAL NOTES

USE AN EXPANSION JOINT FILLER MEETING THE REQUIREMENTS OF STANDARD SPECIFICATION 415.



SKEWED INTERSECTION

CONCRETE PAVEMENT
STEEL REINFORCEMENT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

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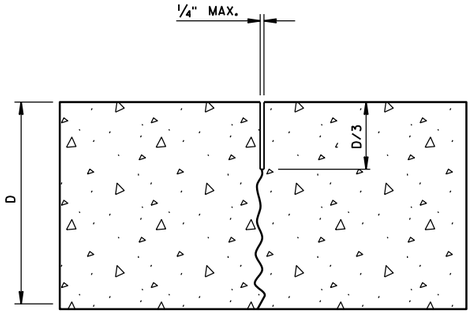
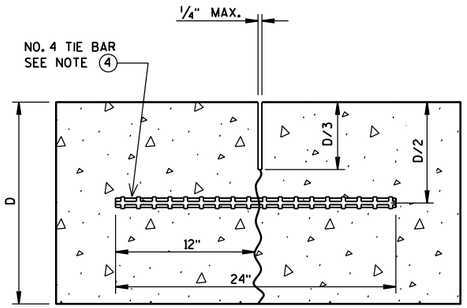
S.D.D. 13 C 18-2b



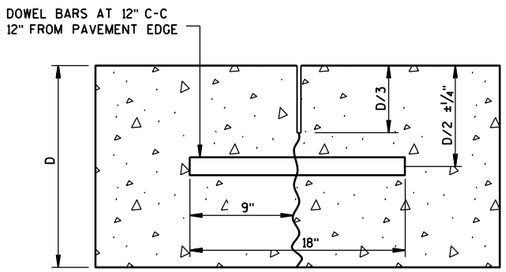
UNDOWELED-TRANSVERSE



TIED LONGITUDINAL

GENERAL NOTES

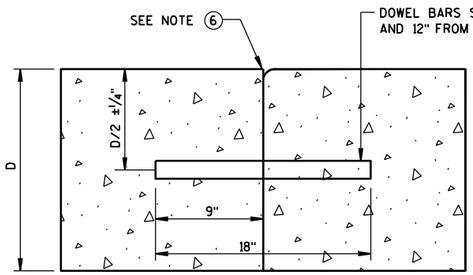
1. USE DOWELED EXPANSION JOINTS ON SIDE ROADS AT INTERSECTIONS (TO ISOLATE THE SIDE ROAD FROM THE THROUGH STREET) IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH.
2. SPACE CONTRACTION JOINTS IN ACCORDANCE WITH 13C4, 13C11 OR 13C13.
3. LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.
4. SPACE TIE BARS AT LONGITUDINAL CONSTRUCTION OR CONTRACTION JOINTS IN ACCORDANCE WITH SDD 13C1.
5. CONSTRUCTION JOINTS CAN BE FORMED OR SAWED.
6. IF JOINT IS FORMED, PROVIDE A 1/4-INCH RADIUS.



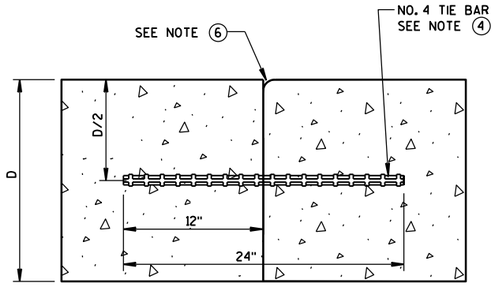
DOWELED-TRANSVERSE

CONTRACTION JOINTS

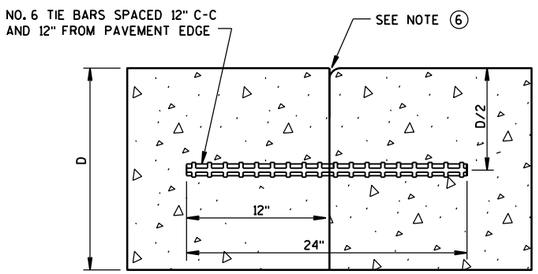
SEE NOTE (2)



DOWELED TRANSVERSE

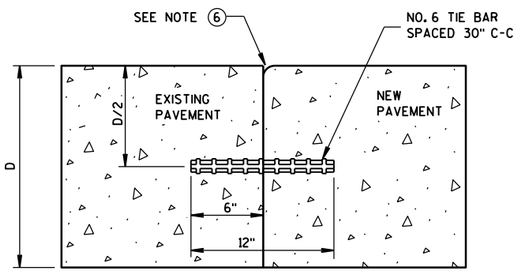


TIED LONGITUDINAL



TIED TRANSVERSE

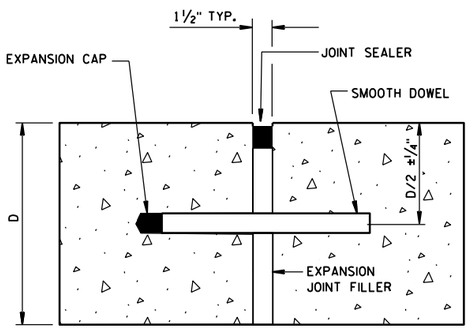
(FOR USE ON NON-DOWELED PAVEMENTS ONLY)



TIED LONGITUDINAL TO EXISTING

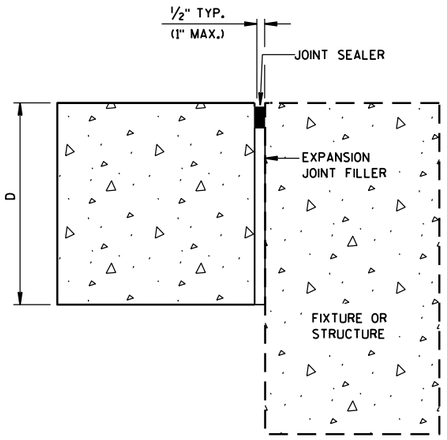
CONSTRUCTION JOINTS

SEE NOTE (5)



DOWELED-TRANSVERSE

SEE NOTE (1)



UNTIED-LONGITUDINAL

EXPANSION JOINTS

CONCRETE PAVEMENT
JOINT TYPES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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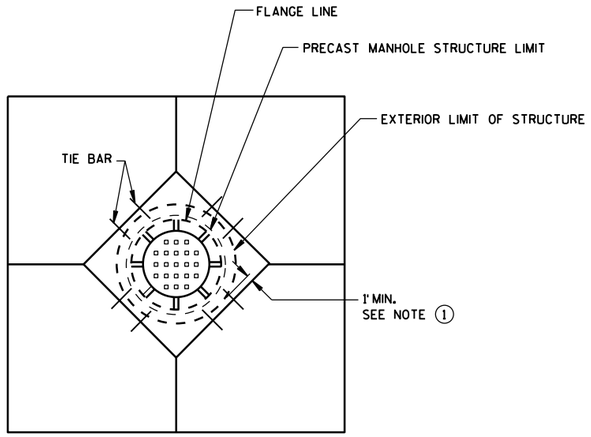
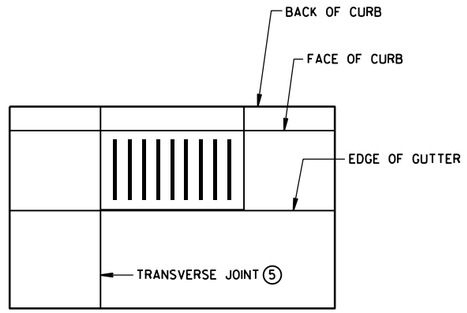
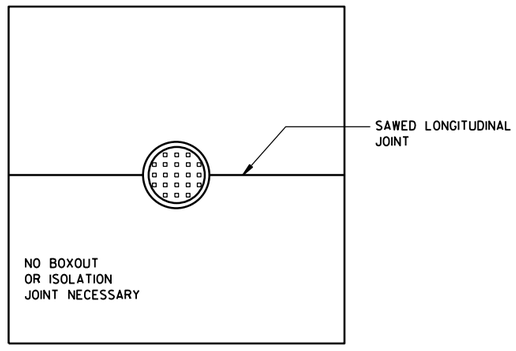
S.D.D. 13 C 18-2c



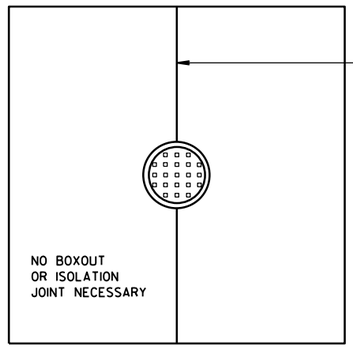
DIAGONAL MANHOLE BOXOUT FOR CONSTRUCTION JOINTS



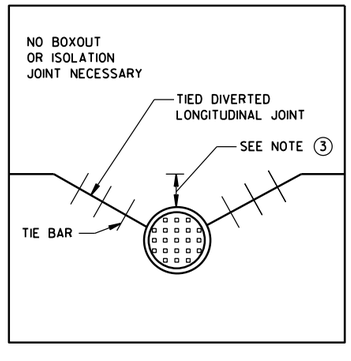
INLET WITH TRANSVERSE JOINT



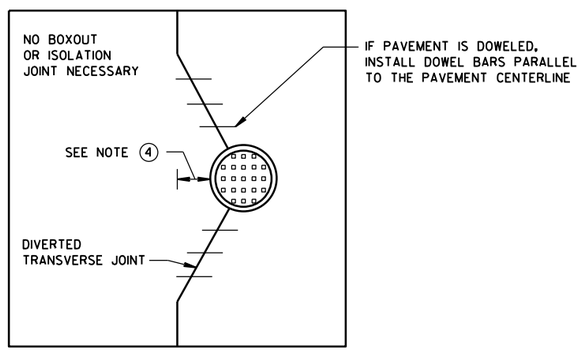
MANHOLE WITH LONGITUDINAL JOINT



MANHOLE WITH TRANSVERSE JOINT



MANHOLE WITH DIVERTED LONGITUDINAL CONTRACTION JOINT



MANHOLE WITH DIVERTED TRANSVERSE CONTRACTION JOINT

GENERAL NOTES

(1) USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1-FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.

(2) ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.

(3) IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS, DIVERT THE LONGITUDIAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.

(4) IF DISTANCE FROM THE EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS 4 FEET OR LESS, REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REBAR REINFORCEMENT AROUND THE MANHOLE.

(5) ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.

CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

5-3-2013 DATE

/S/ Deb Bischoff PAVEMENT POLICY & DE

NEER

FHWA

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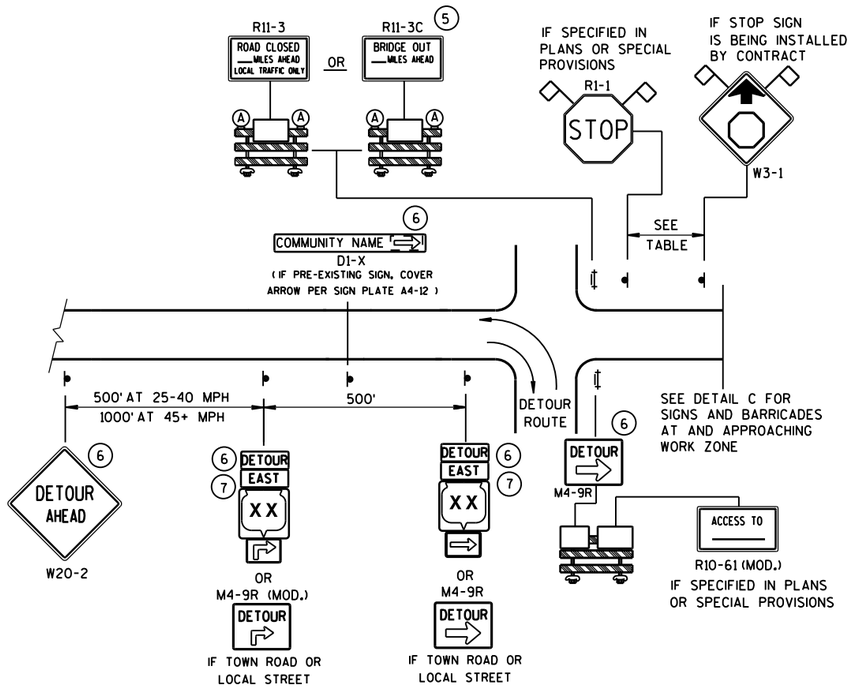
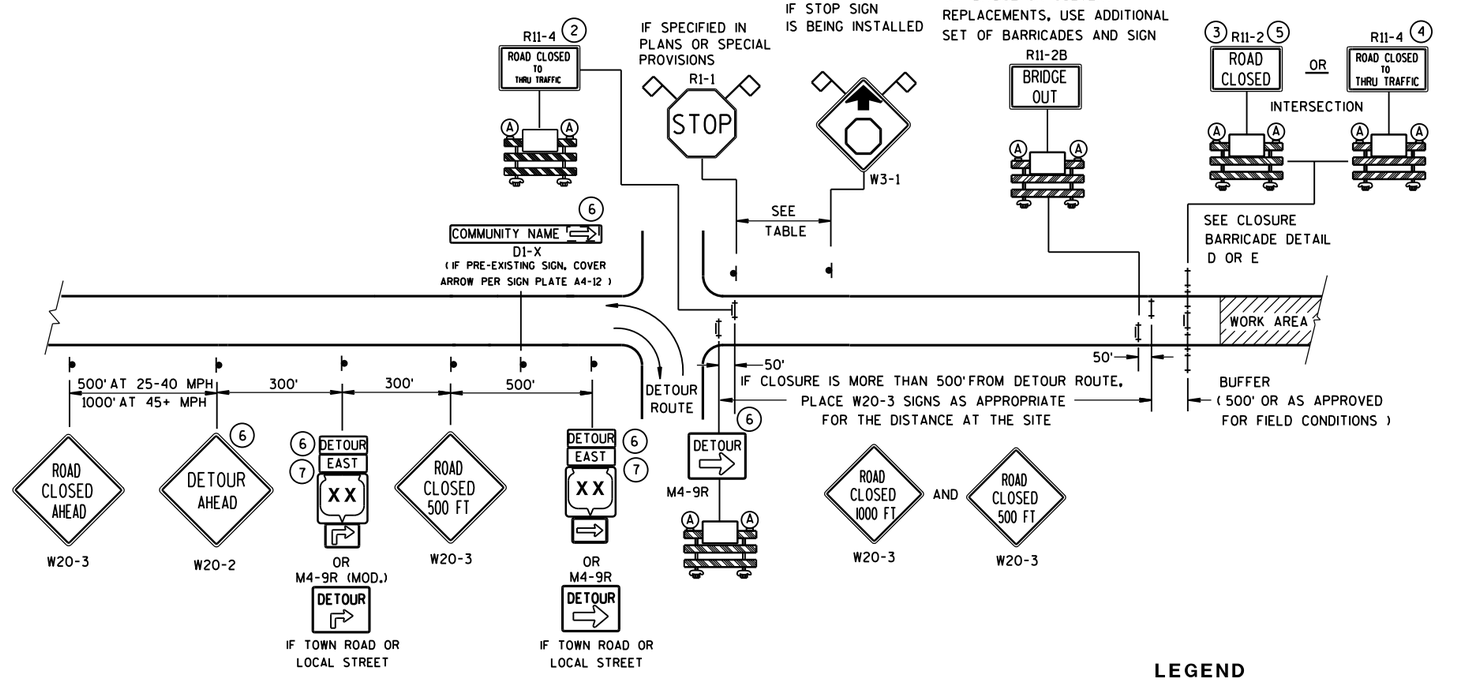
S.D.D. 13 C 18-2d



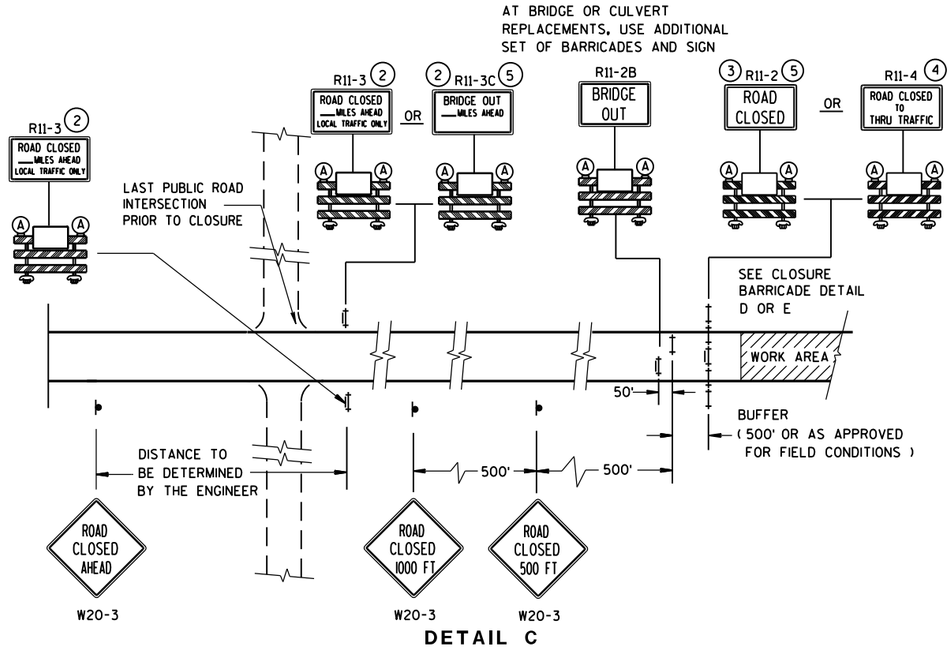
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "**b**" FOR GENERAL NOTES AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- DETOUR / EAST — M4-8 / M3-X
- XX — M1-4; COUNTY XX — M1-5A; XX — M1-6
- M05-1 OR M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

8/2013 DATE

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

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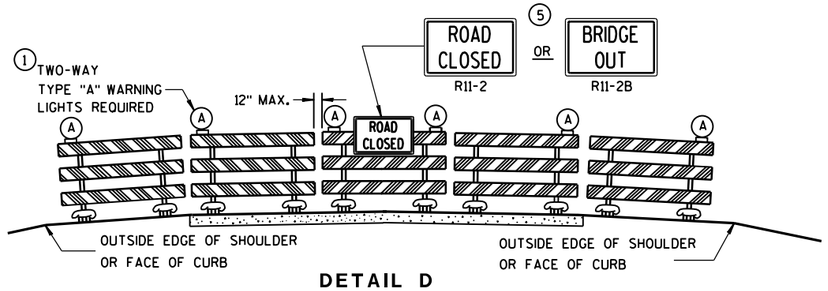
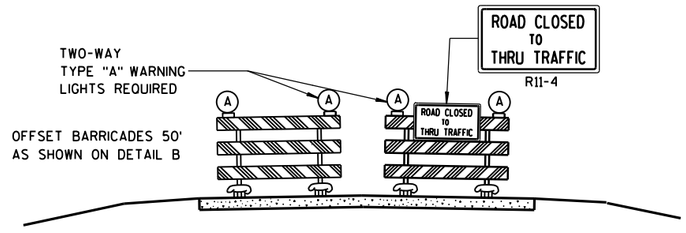
S.D.D. 15 C 2-5a



DETAIL D

ROAD CLOSURE BARRICADE DETAIL

APPROACH VIEW



DETAIL E

LANE CLOSURE BARRICADE DETAIL

APPROACH VIEW

SEE SDD 15C2-SHEET "**a**" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

8/2013 DATE

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

6

S.D.D. 15 C 2-5b

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

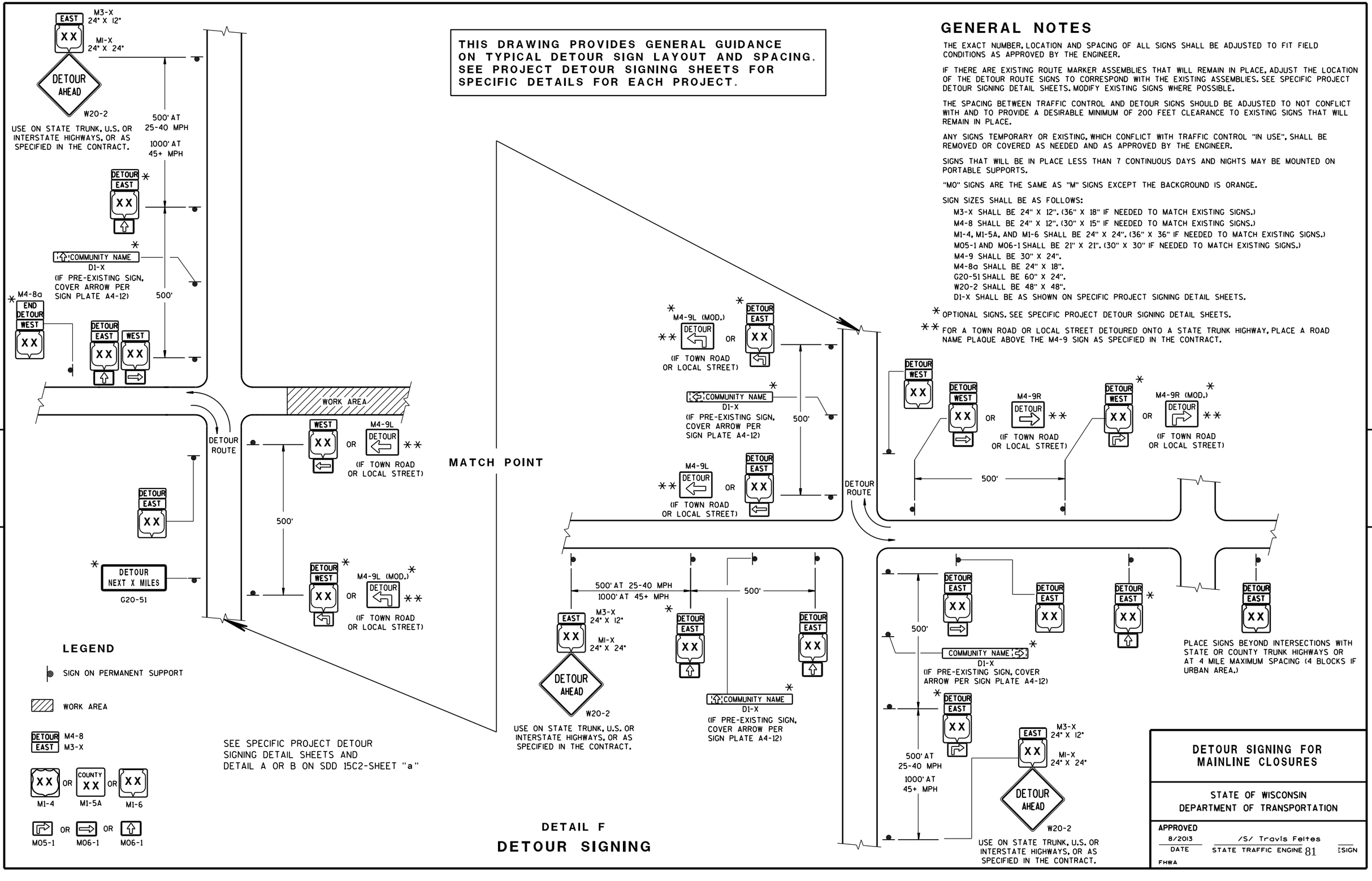
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-9 SHALL BE 30" X 24".
- M4-8a SHALL BE 24" X 18".
- G20-51 SHALL BE 60" X 24".
- W20-2 SHALL BE 48" X 48".
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



DETAIL F

DETOUR SIGNING

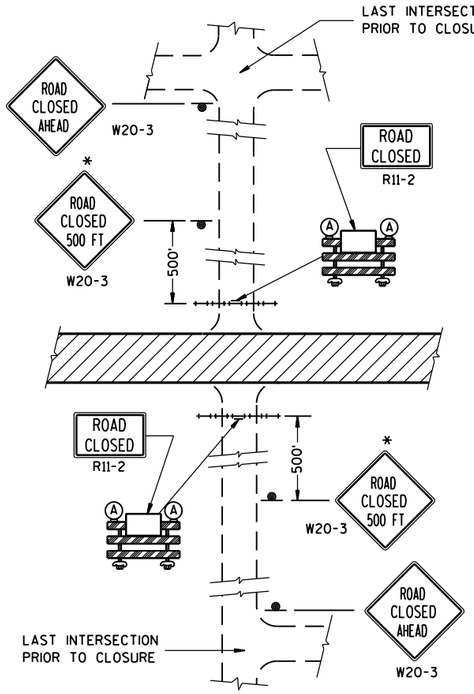
DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 DATE
/S/ Travis Feltes
STATE TRAFFIC ENGINEER

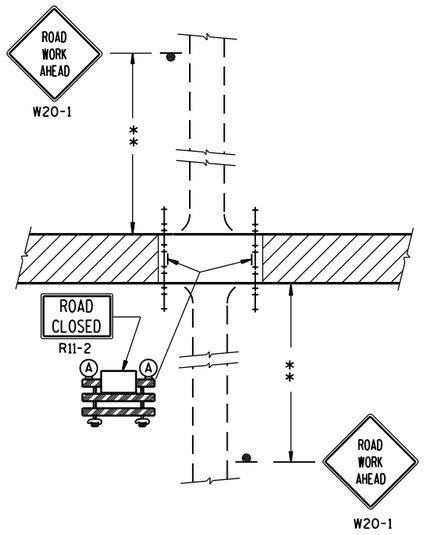
FHWA

6 S.D.D. 15 C 2-5c



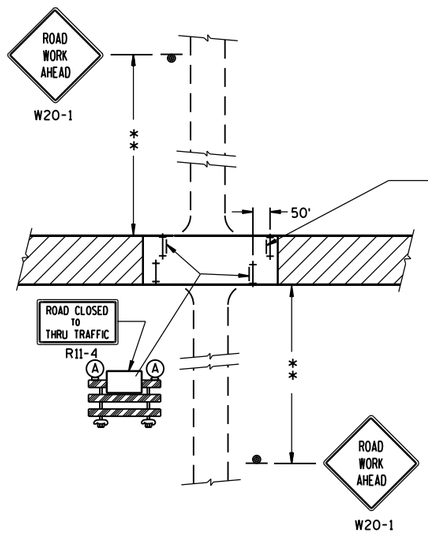
DETAIL 1

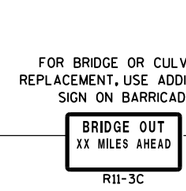
(NO ACCESS TO PROJECT)



DETAIL 2

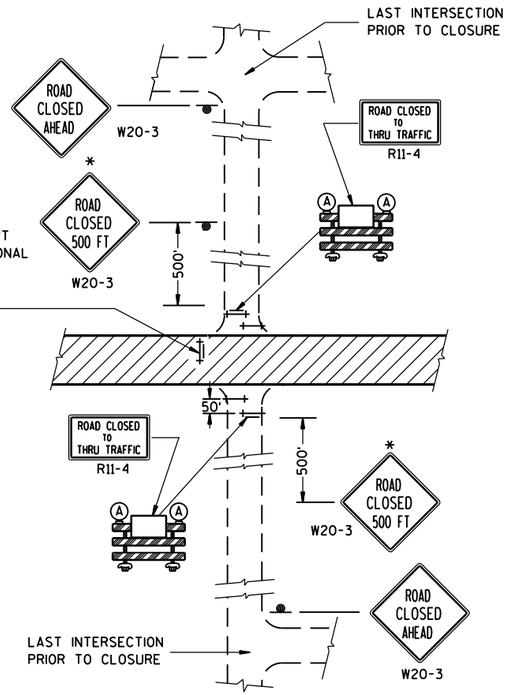
(PUBLIC CROSS-TRAFFIC MAINTAINED. NO ACCESS TO PROJECT).





DETAIL 3

(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 4

(CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 C 3-2

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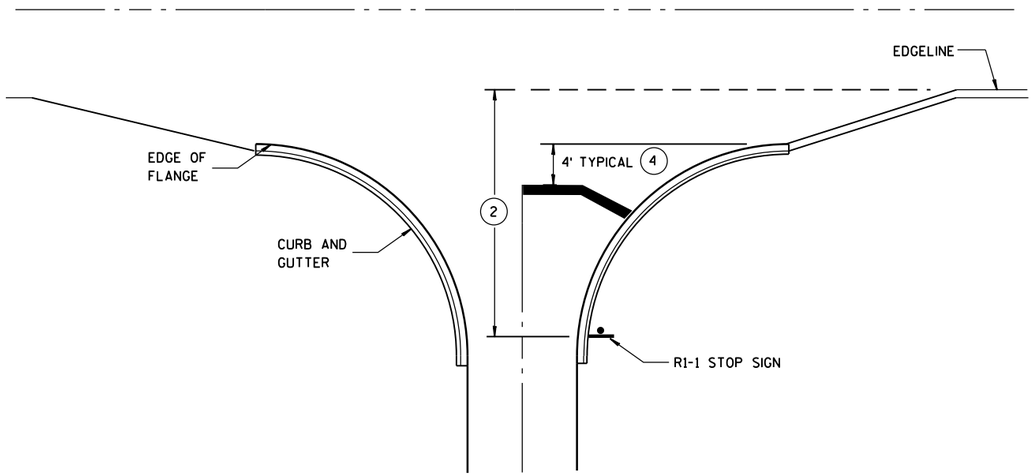
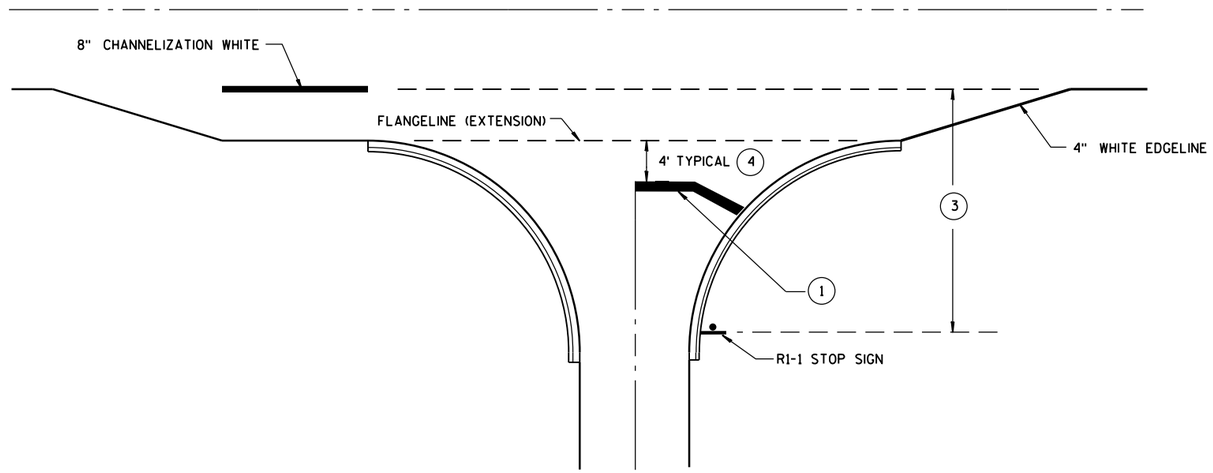
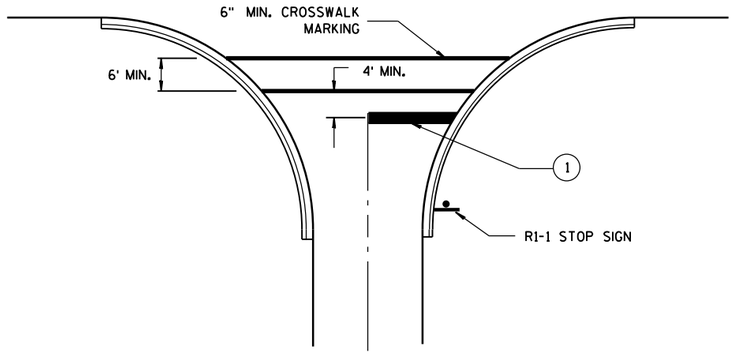
S.D.D. 15 C 3-2



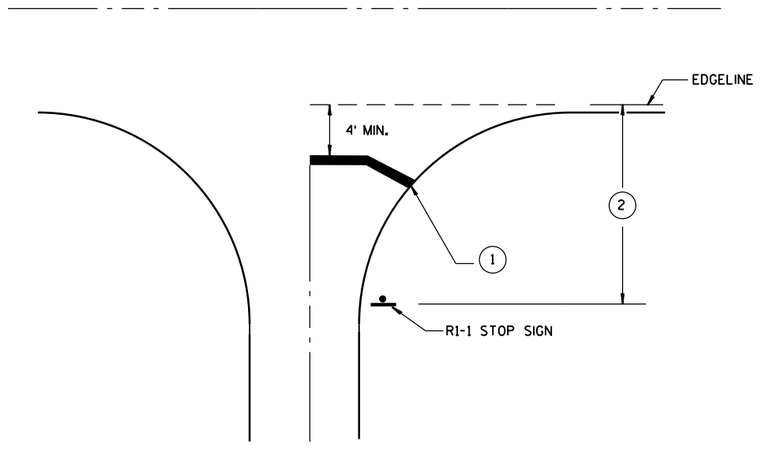
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

GENERAL NOTES

1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE PROJECT ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE THAN NO STOP LINE IS REQUIRED.
3. IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION THAN NO STOP LINE IS REQUIRED.
4. MOVE CLOSER TO EDGE OF TRAVEL LANE AS NEEDED FOR VISIBILITY AND SIGHT LINES.

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

4/30/2013
DATE

/S/ Travis Feltes
STATE TRAFFIC E

FHWA

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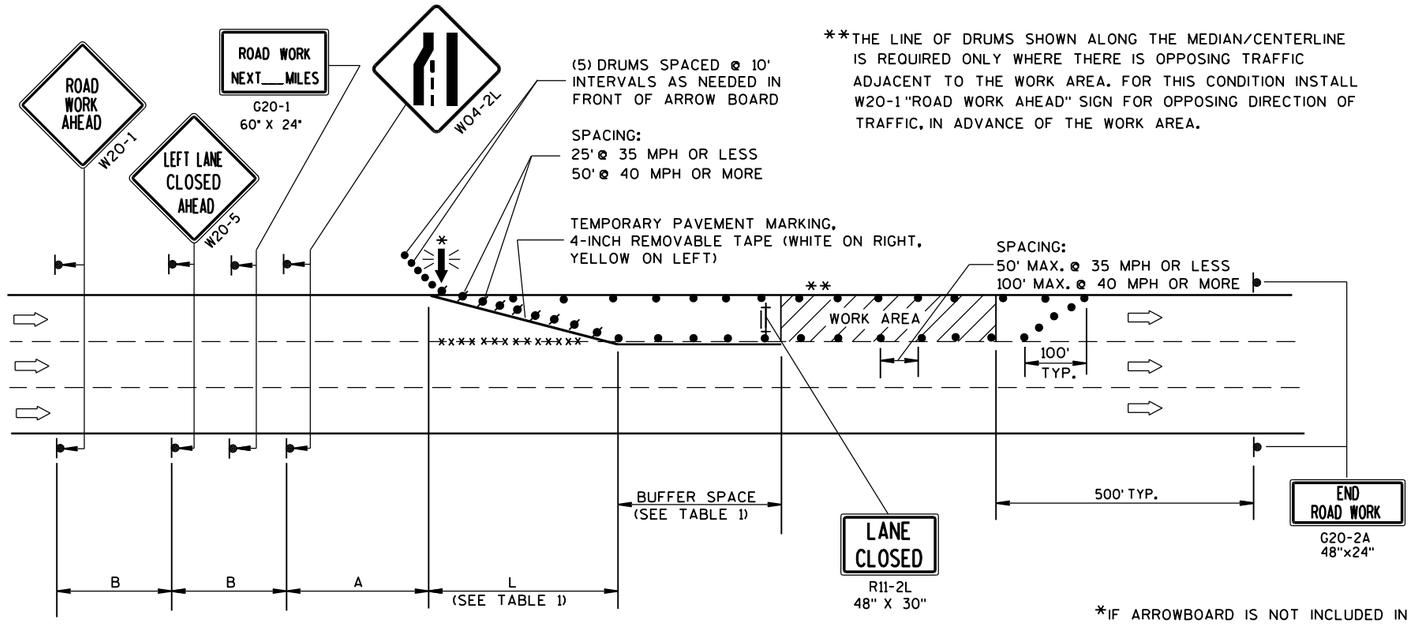
S.D.D. 15 C 33-1



**THE LINE OF DRUMS SHOWN ALONG THE MEDIAN/CENTERLINE IS REQUIRED ONLY WHERE THERE IS OPPOSING TRAFFIC ADJACENT TO THE WORK AREA. FOR THIS CONDITION INSTALL W20-1 "ROAD WORK AHEAD" SIGN FOR OPPOSING DIRECTION OF TRAFFIC, IN ADVANCE OF THE WORK AREA.

B=400' AT 25-30 MPH
700' AT 35-40 MPH
1000' AT 45-55 MPH

A=200' AT 25-30 MPH
350' AT 35-40 MPH
500' AT 45-55 MPH

(PLACE BARRICADE AND SIGN APPROX. EVERY 1000' ACROSS THE CLOSED LANE)

*IF ARROWBOARD IS NOT INCLUDED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE A TYPE III BARRICADE WITH W01-6 SIGN IN THE LANE CLOSURE TAPER.

W01-6
48"x24"

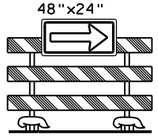


TABLE 1
TAPER AND BUFFER SPACE FOR 12' LANE WIDTH

S	L	BUFFER SPACE
25	125'	55'
30	180'	85'
35	245'	120'
40	320'	170'
45	540'	220'
50	600'	280'
55	660'	335'

FOR LANE WIDTH OTHER THAN 12':

L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

L = TAPER LENGTH IN FEET

S = NON-CONSTRUCTION SPEED LIMIT (MPH)

W = WIDTH OF LANE CLOSURE

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.

W20-1, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Feb. 2015 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 D 20-3

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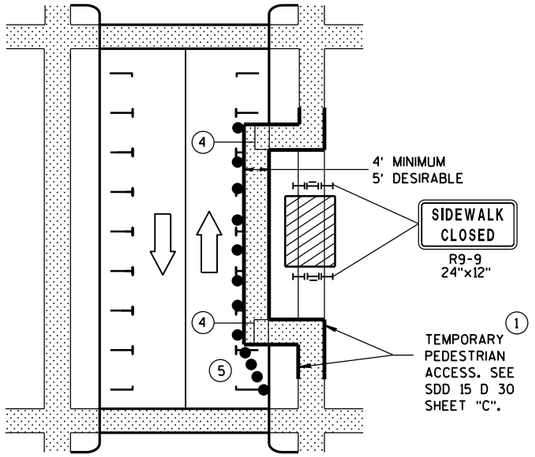
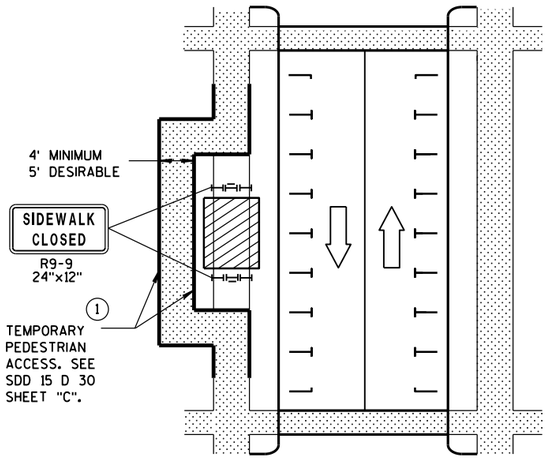
S.D.D. 15 D 20-3

NOTE: MAY BE USED ON ROADWAY WITH POSTED SPEED OF LESS THAN 40 MPH.

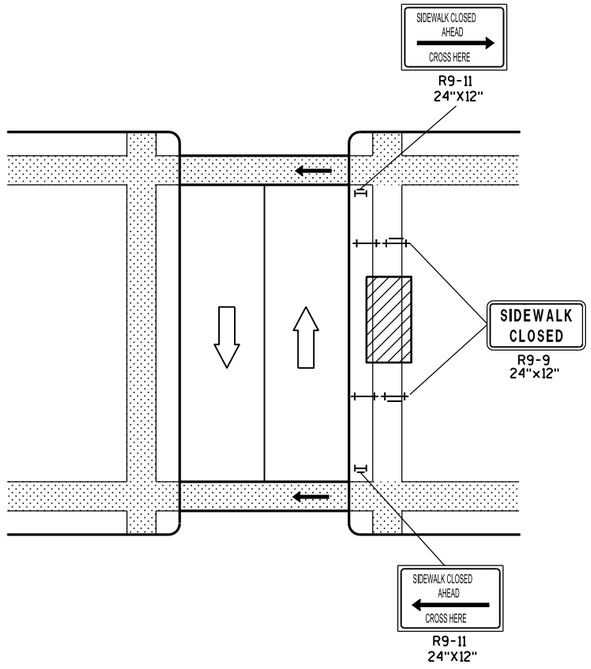


MID-BLOCK SIDEWALK CLOSURE IN PARKING LANE

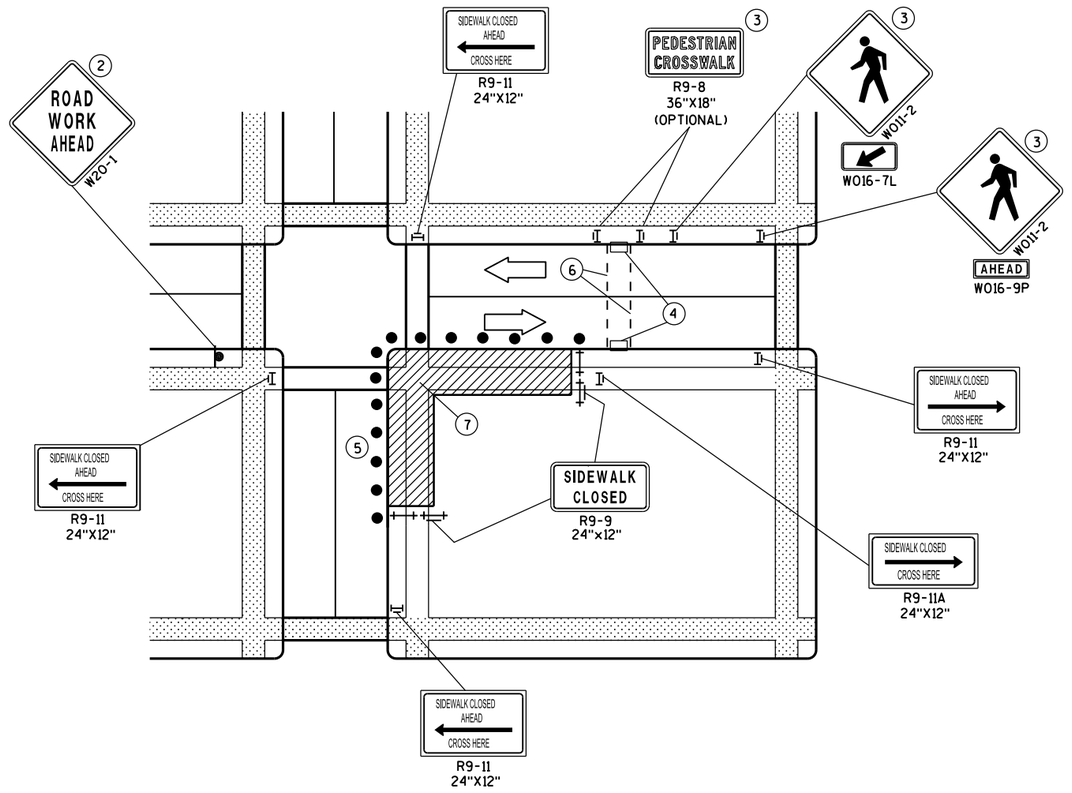
NOTE: LAYOUT SAME AS ABOVE.



SIDEWALK DIVERSION



MID-BLOCK SIDEWALK CLOSURE



CORNER SIDEWALK CLOSURE WITH TEMPORARY CROSSWALK

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

(1) IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.

(2) "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.

(3) IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND W011-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.

(4) TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".

(5) DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.

(6) TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.

(7) LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

LEGEND

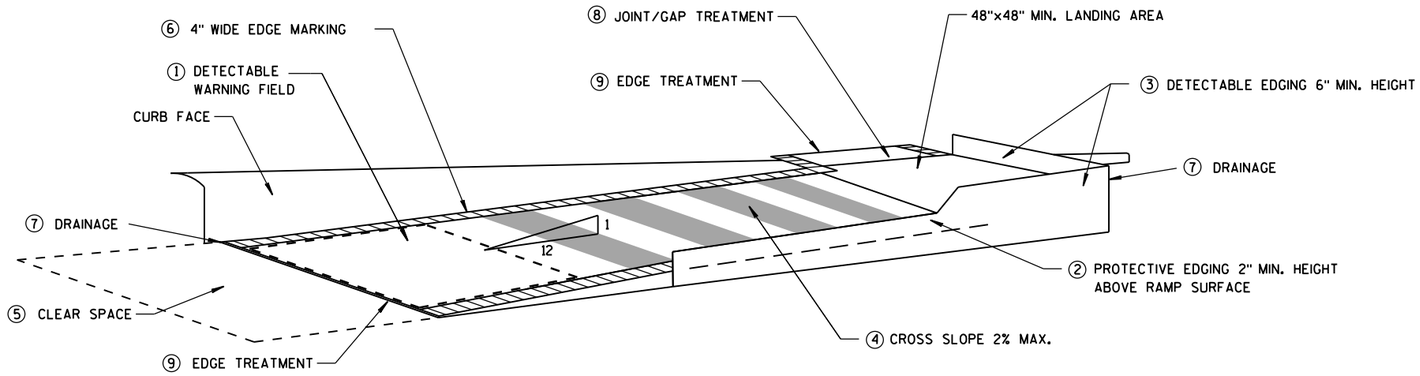
- SIGN ON PERMANENT SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- PEDESTRIAN CHANNELIZATION DEVICE
- TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)
- TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)
- DIRECTION OF TRAFFIC
- TRAFFIC CONTOL DRUM

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOR

6

S.D.D. 15 D 30-2a



TEMPORARY CURB RAMP PARALLEL TO CURB

GENERAL NOTES

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

① CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 8D5 SHEET "E".

② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

④ CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

⑤ CLEAR SPACE OF 48"X48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

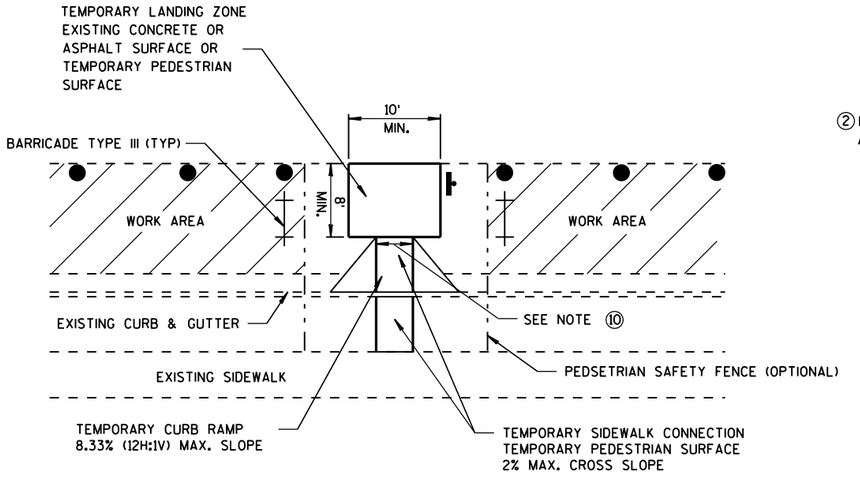
⑥ THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.

⑦ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

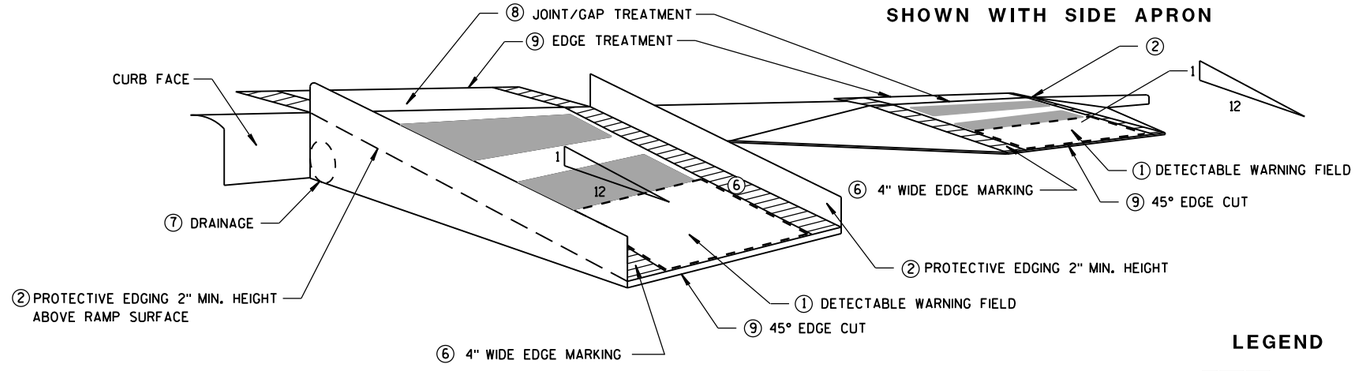
⑧ LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

⑨ CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

⑩ 5' WIDE MIN. WITH PEDSETRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.



TEMPORARY BUS STOP PAD



SHOWN WITH SIDE APRON

SHOWN WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP PERPENDICULAR TO CURB

LEGEND

WORK AREA

TYPE III BARRICADE

TRAFFIC CONTOL DRUM

TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

March 2015 DATE — /S/ Travis Feltes STATE TRAFFIC ENGINEER — SIGN

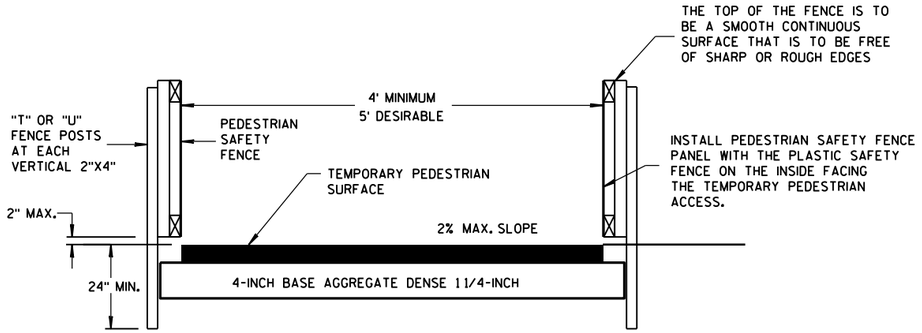
FHWA

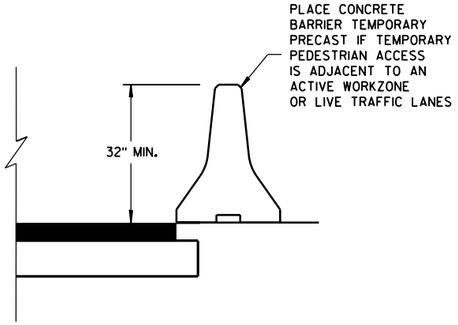
6

S.D.D. 15 D 30-2b

6

S.D.D. 15 D 30-2b

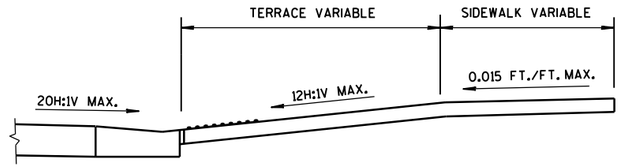




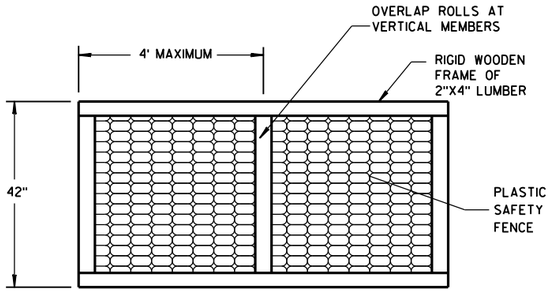
TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

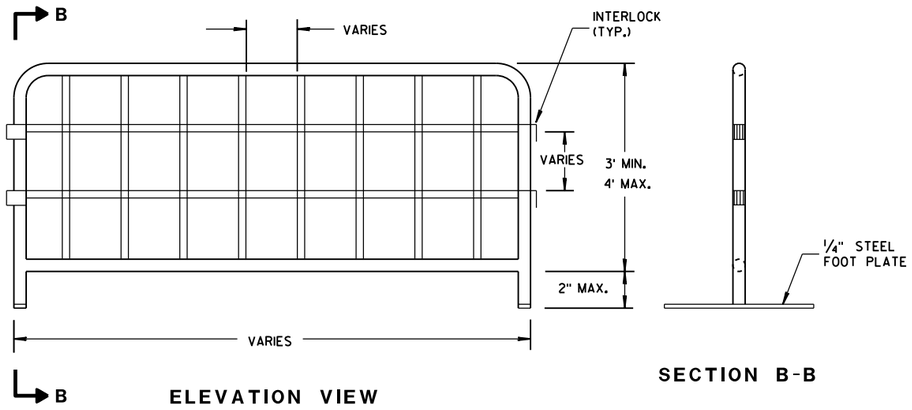
(1) INTERCHANGEABLE WITH THE PEDESTRIAN SAFETY FENCE.



SECTION A-A



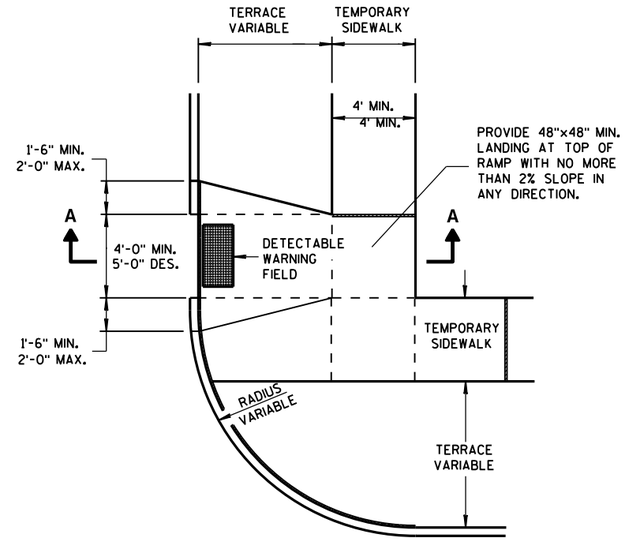
PEDESTRIAN SAFETY FENCE



ELEVATION VIEW

SECTION B-B

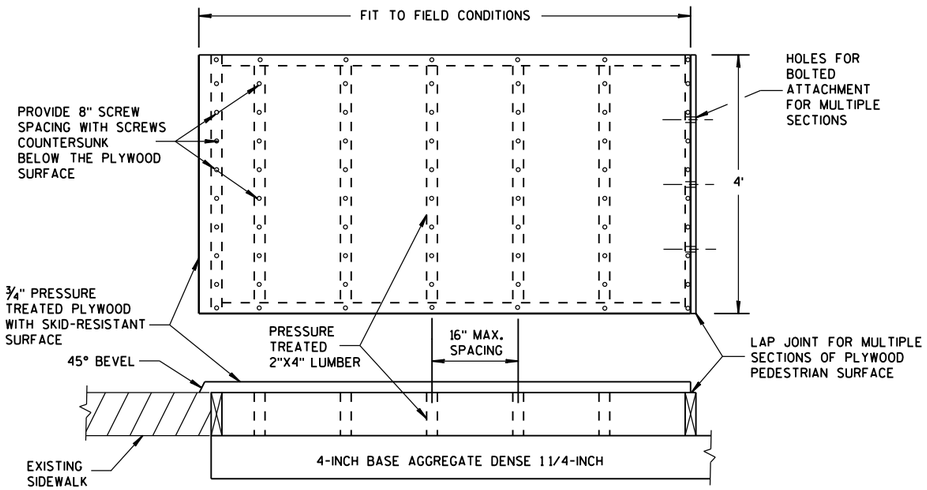
TEMPORARY PEDESTRIAN STEEL BARRICADE (1)



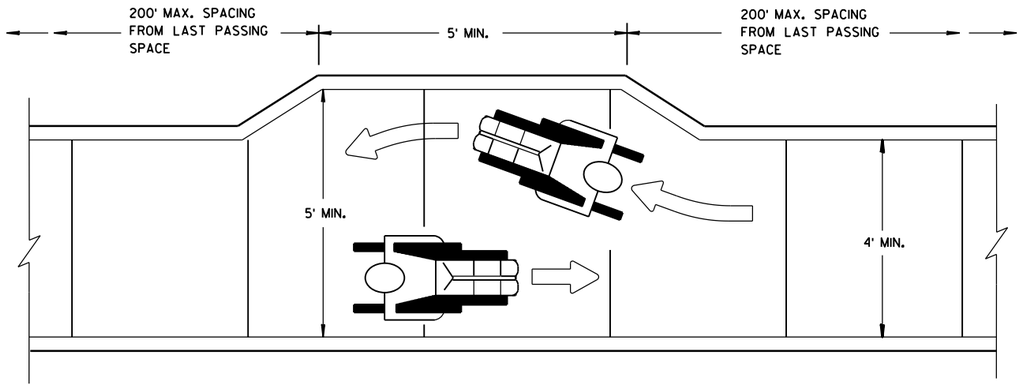
PLAN VIEW

TEMPORARY TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)



TEMPORARY PEDESTRIAN SURFACE PLYWOOD



NARROW SIDEWALK PASSING DETAIL

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2015 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER — SIGN
FHWA

6

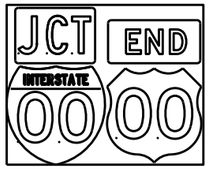
S.D.D. 15 D 30-2c

6

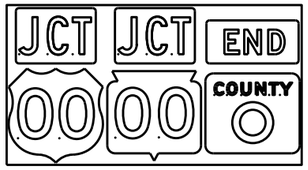
S.D.D. 15 D 30-2c

TYPICAL ASSEMBLIES

J1-1

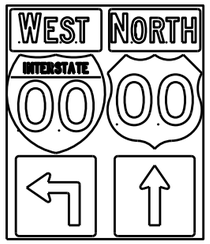


J1-2

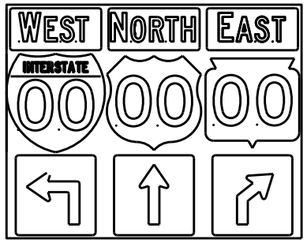


J1-3

J2-1



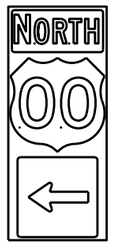
J2-2



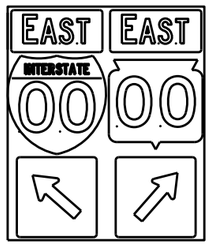
J2-3

JV

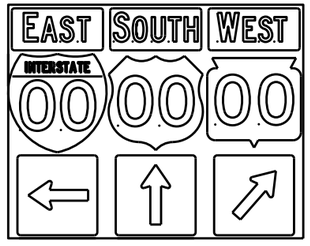
(Typical Vertical J-Assembly
See Note 10 and 11)



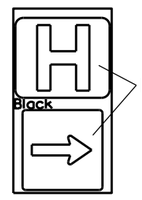
J3-1



J3-2

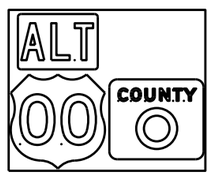


J3-3

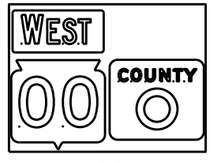


JH-1

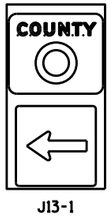
J4-1



J4-2



J4-2



J13-1

J12-1

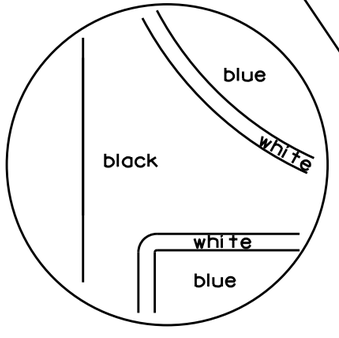
J32-1

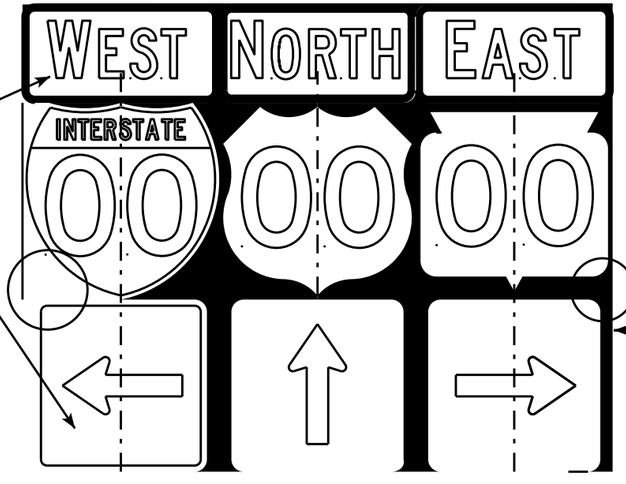
J33-1

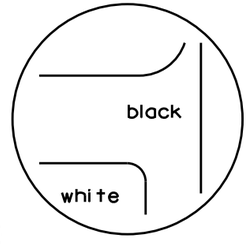
J23-1

J22-1

blue background
with interstate







black background

NOTES

1. Signs are Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Black Non-reflective
   Message - see Note 5
3. Message Series - See Note 5
4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inchs (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
10. All Vertical J Assemblies are given a Sign Code of JV
11. For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

ROUTE MARKERS & COMPONENTS
IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

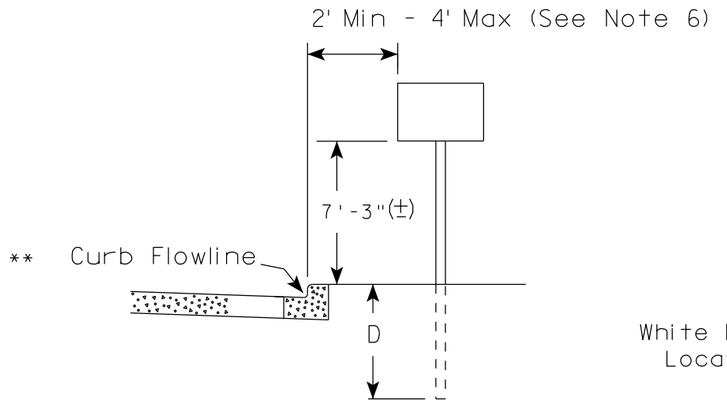
DATE 2/06/14 PL... A2-1S.8

PROJECT NO: | SHEET NO: E

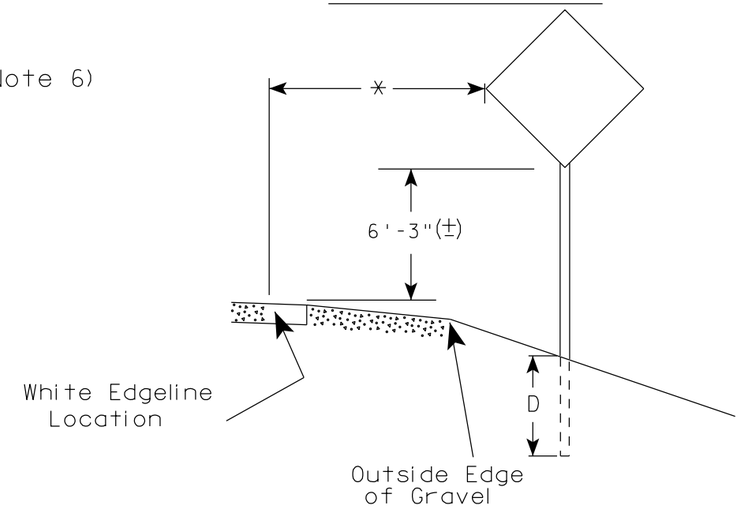
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A21S.DGN PLOT DATE : 06-FEB-2014 14:10 PLOT BY : mscsja PLOT NAME :

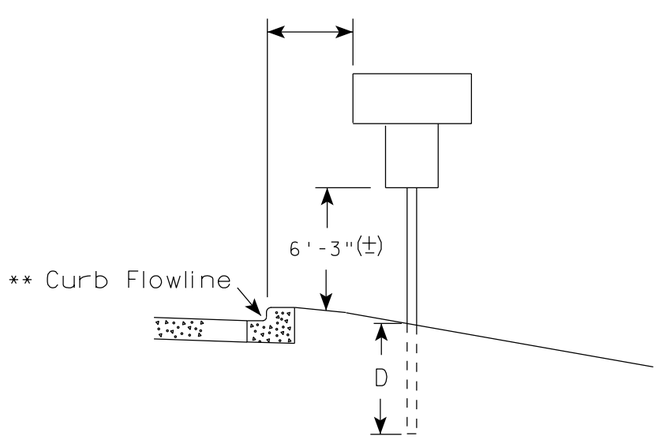
WISDOT/CADDS SHEET 42

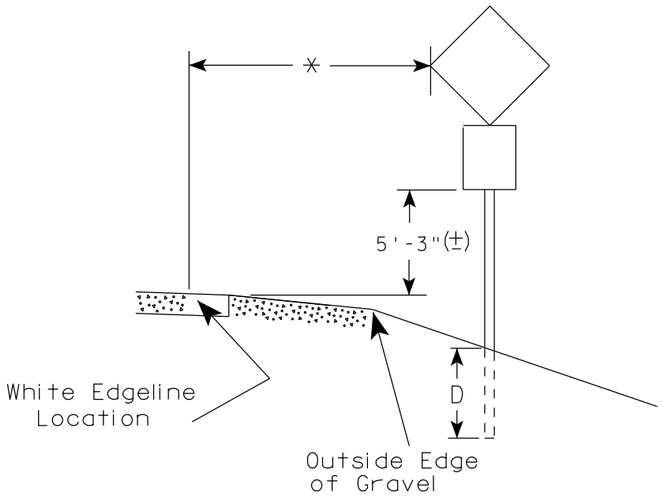
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

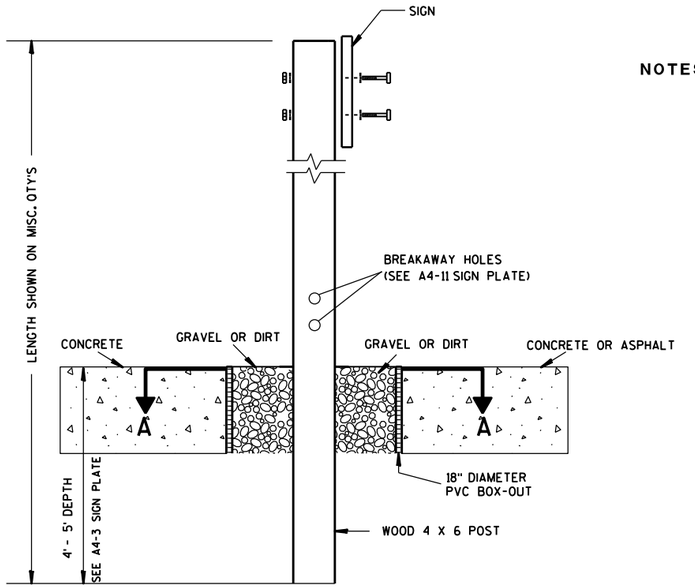
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/12/14 PLATE NO. A4-3.19

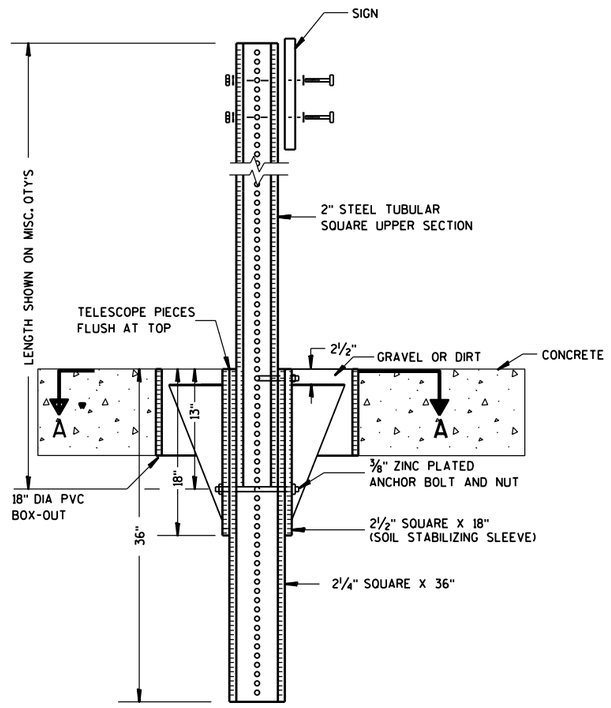
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



ELEVATION VIEW

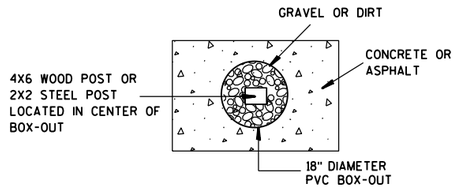
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

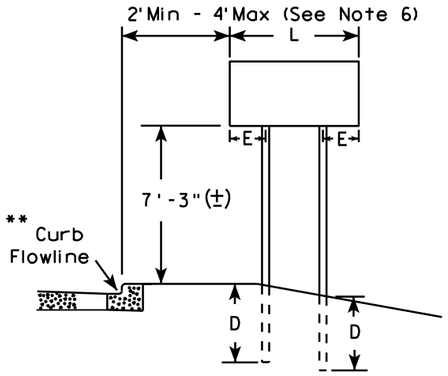
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

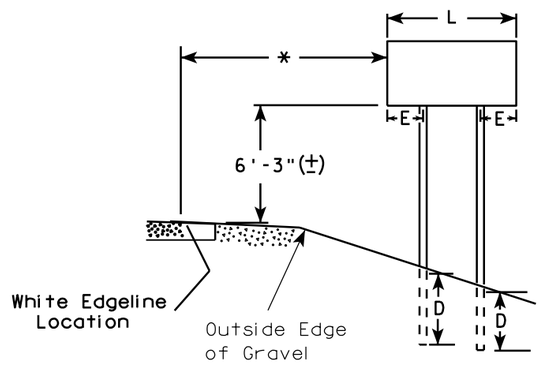
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

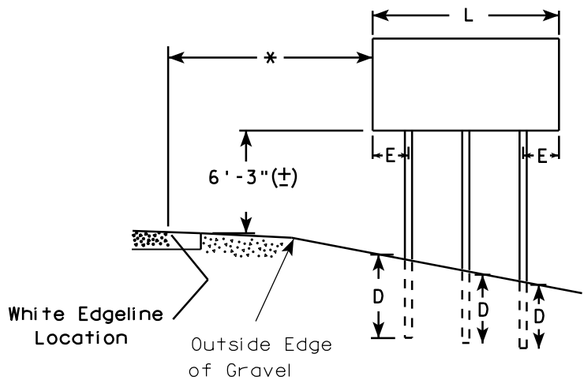
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

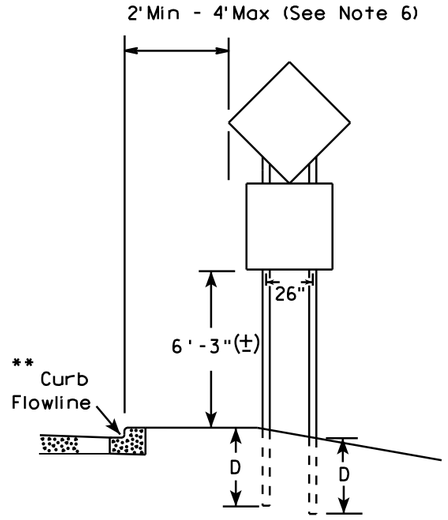
URBAN AREA



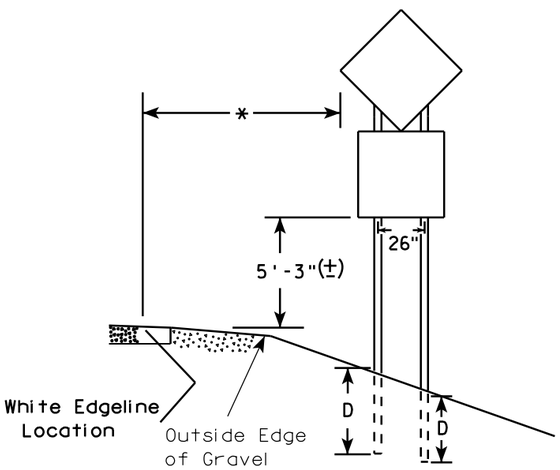
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. Minimum mounting height for J assemblies (A2-1S) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width or less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 120"	L/5

***

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 120" less than 168"	12"

SIGN SHAPE OTHER THAN DIAMOND (FOUR POSTS REQUIRED)	
L	E
168" and greater	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

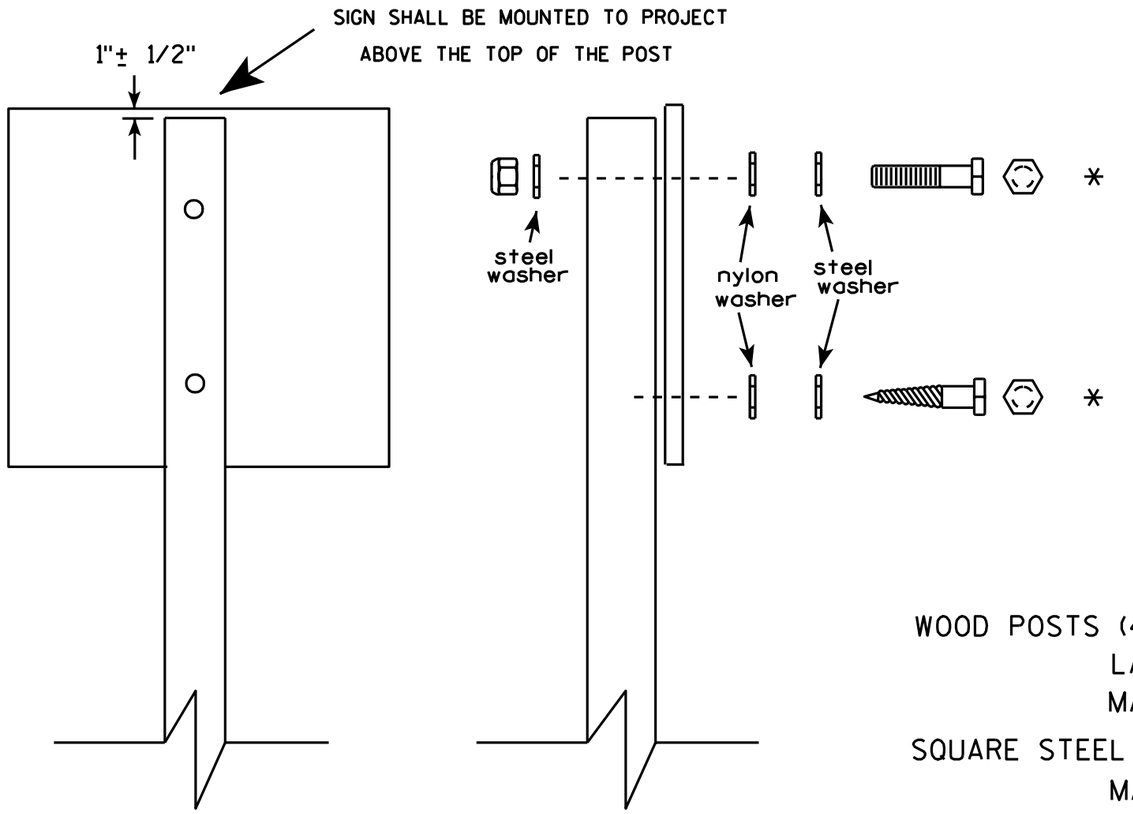
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/12/14 PLATE NO. A4-4 13

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A44.DGN PLOT DATE : 12-NOV-2014 14:01 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 107.021305:1.000000 WISDOT/CADDS SHEET 42



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

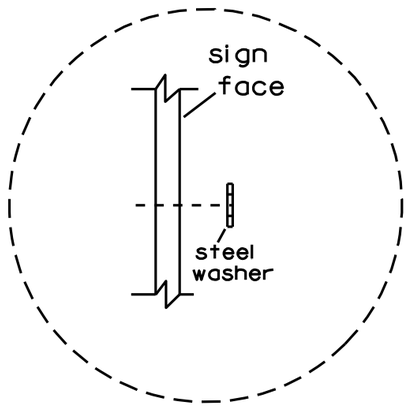
b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

WOOD POSTS (4" x 4" or 4" x 6")
LAG SCREWS - 3/8" X 3"
MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts

SQUARE STEEL POSTS (2" x 2")
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts
RIVETS - 9/32 " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.



Washer Placement when Sign Has Other Than Type H or Type F Face

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/23/10 PLATE NO. A4-8.7

7

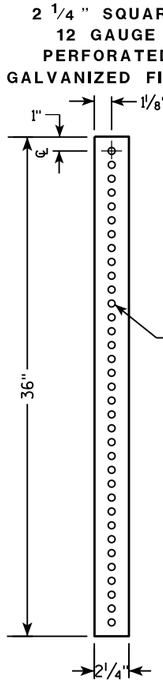
7

PROJECT NO:

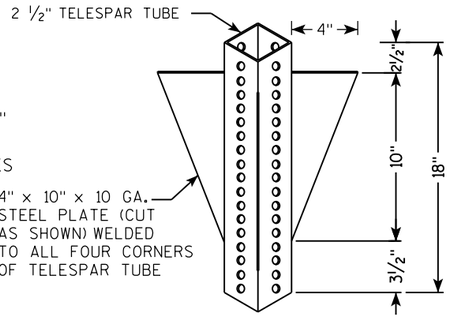
SHEET NO: 92 E

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

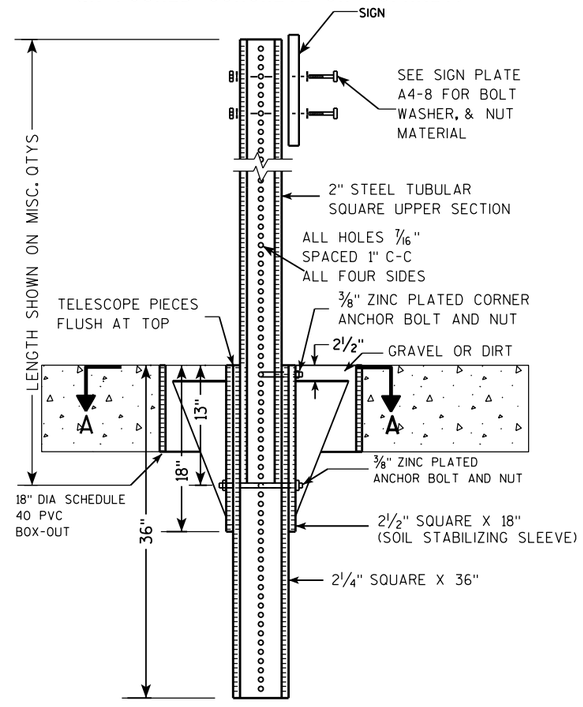
2 ¼ " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



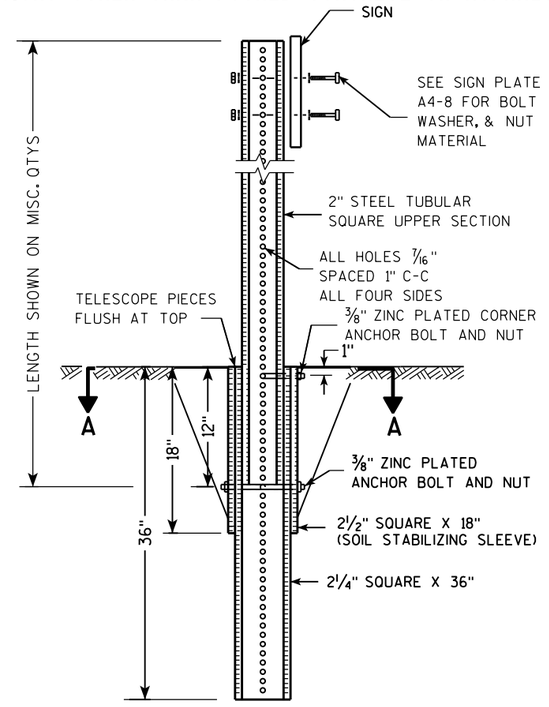
2 ½ " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

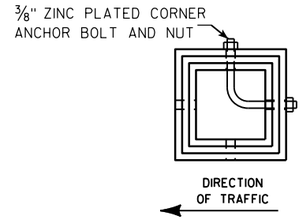


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

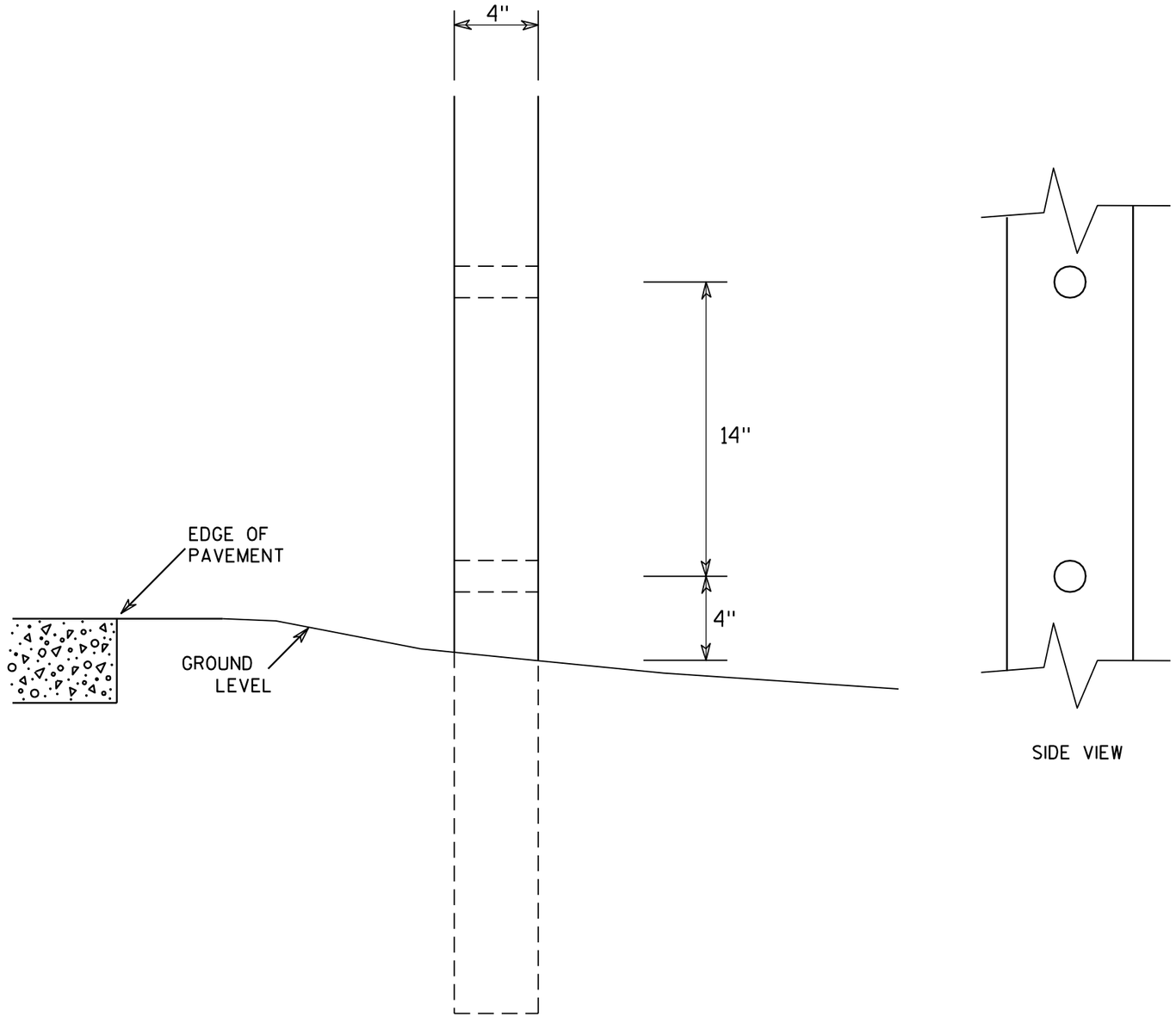
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE 93 A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000

WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

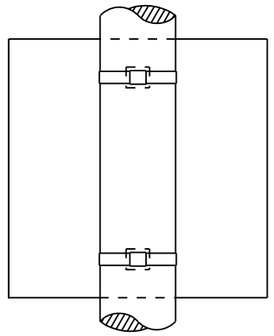
APPROVED Chester J Spang for State Traffic Engineer

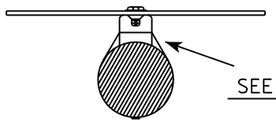
DATE 3/27/97 PLATE NO. A4-11.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 94	E

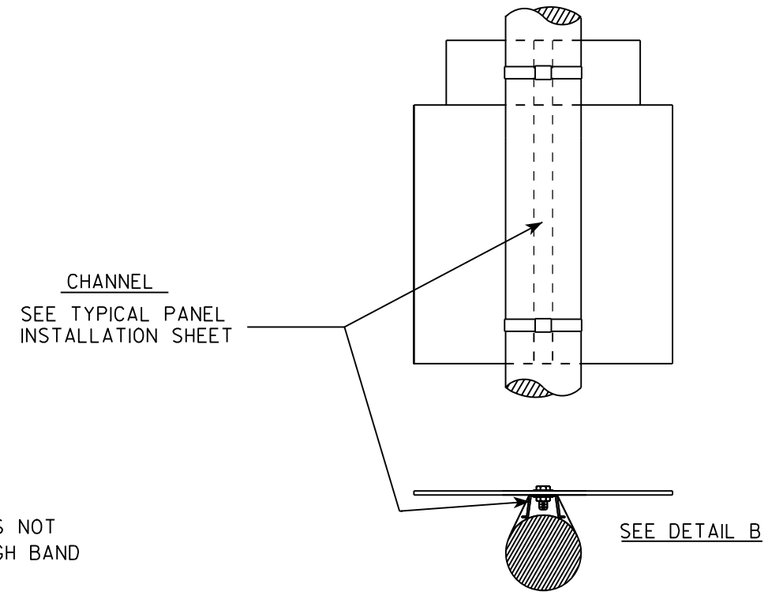
BANDING

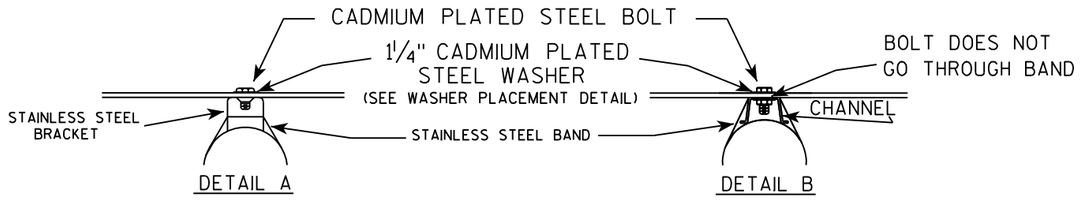
SINGLE SIGN



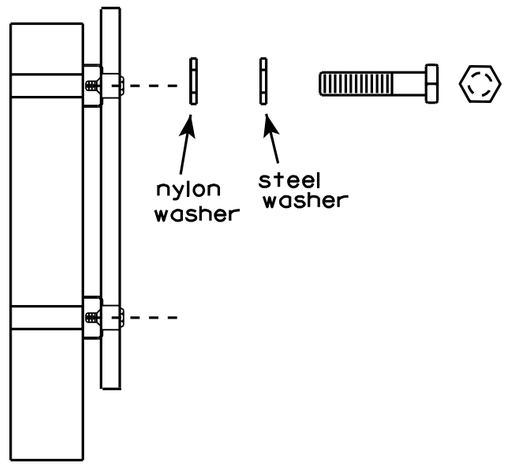


"J" ASSEMBLY





WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.

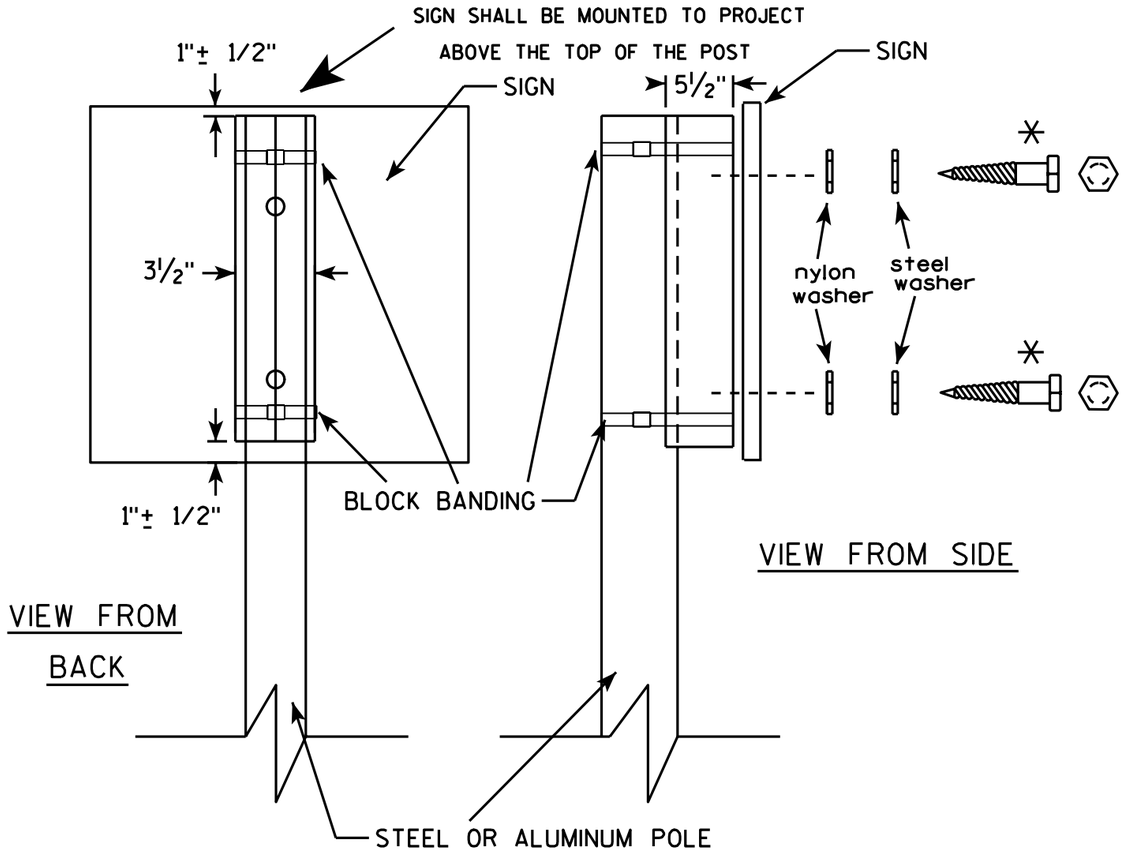
STANDARD SIGN
SIGN BANDING DETAILS

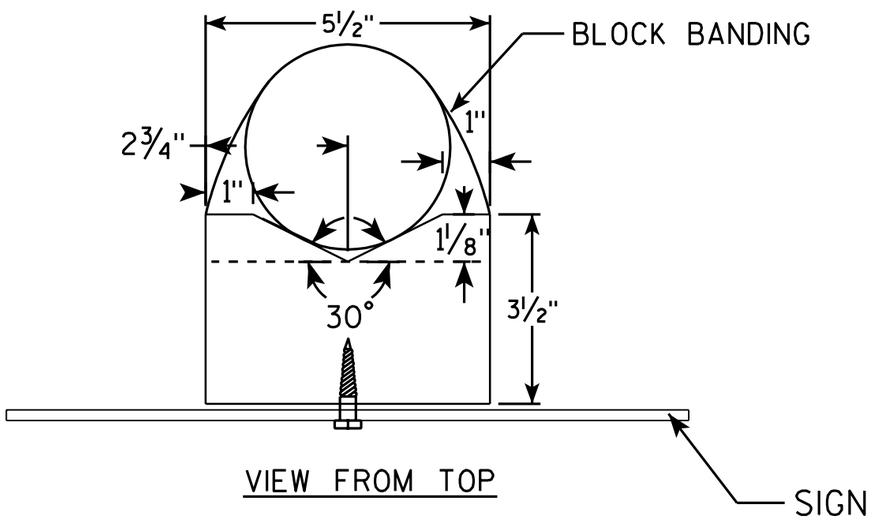
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/16/13 PLATE NO. A5-9.3

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 95	E





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D, or
   b. Cadmium plated in accordance with ASTM Designation : B 766 TYPE 3, Class 12, or
   c. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

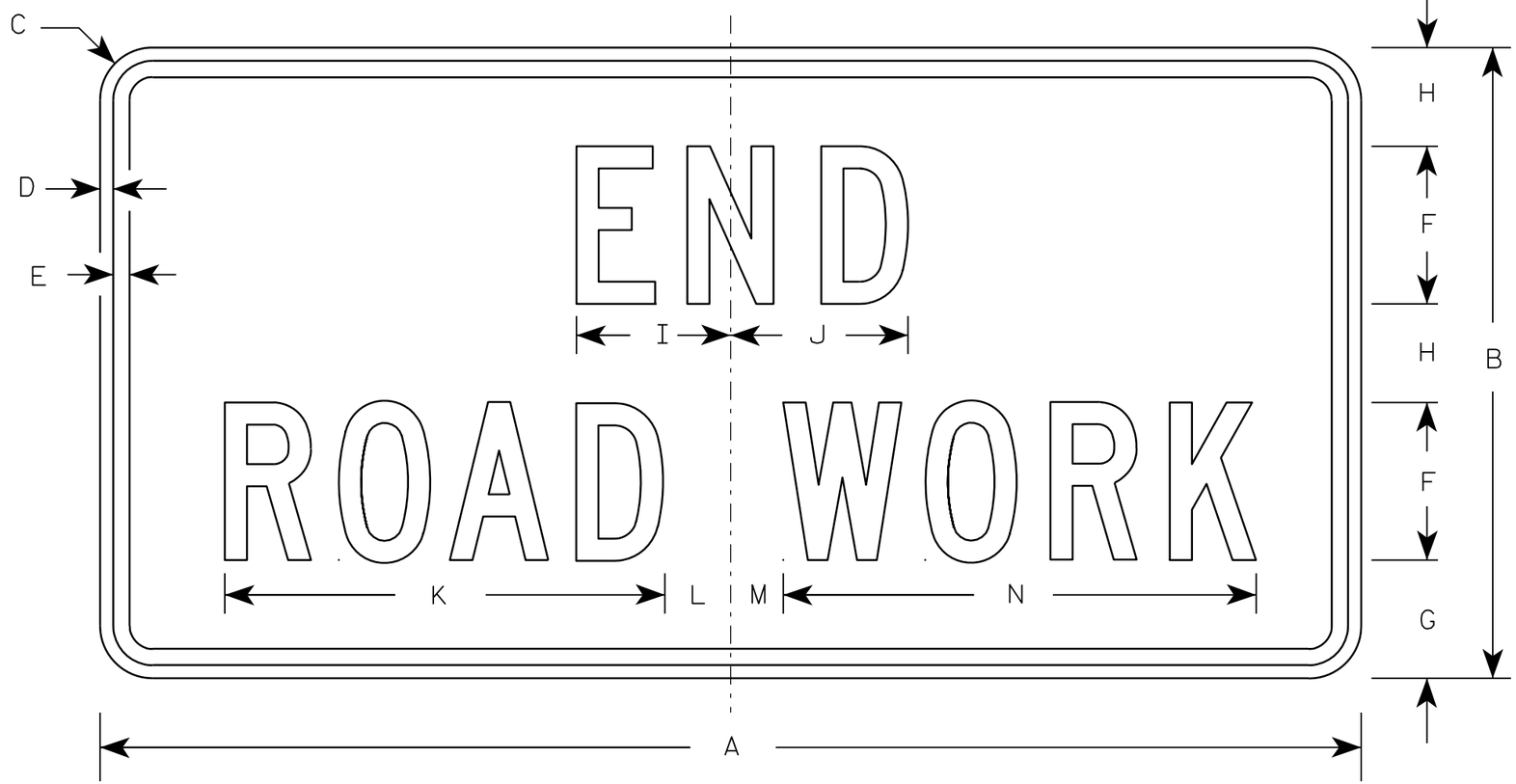
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/12/07 PLATE NO. A5-10.1

PROJECT NO:

SHEET NO: 96 E



G20-2A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

Metric equivalent for this sign is:

SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

STANDARD SIGN
G20-2A

WISCONSIN DEPT OF TRANSPORTATION

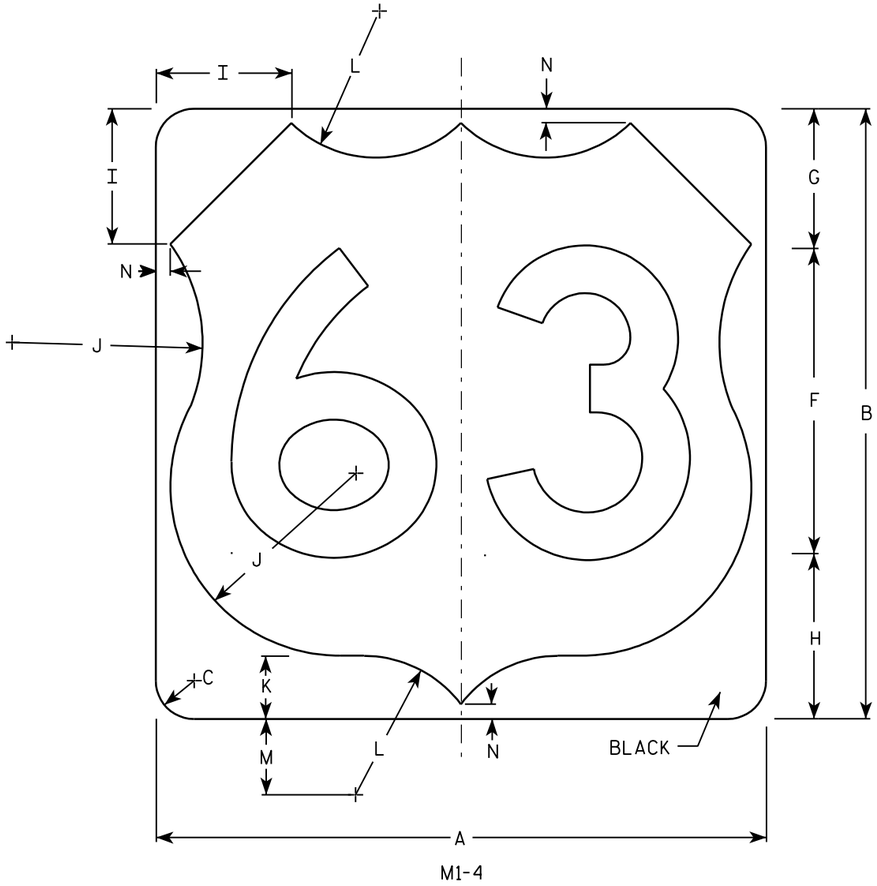
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/30/09 PLATE NO. G20-2A.8

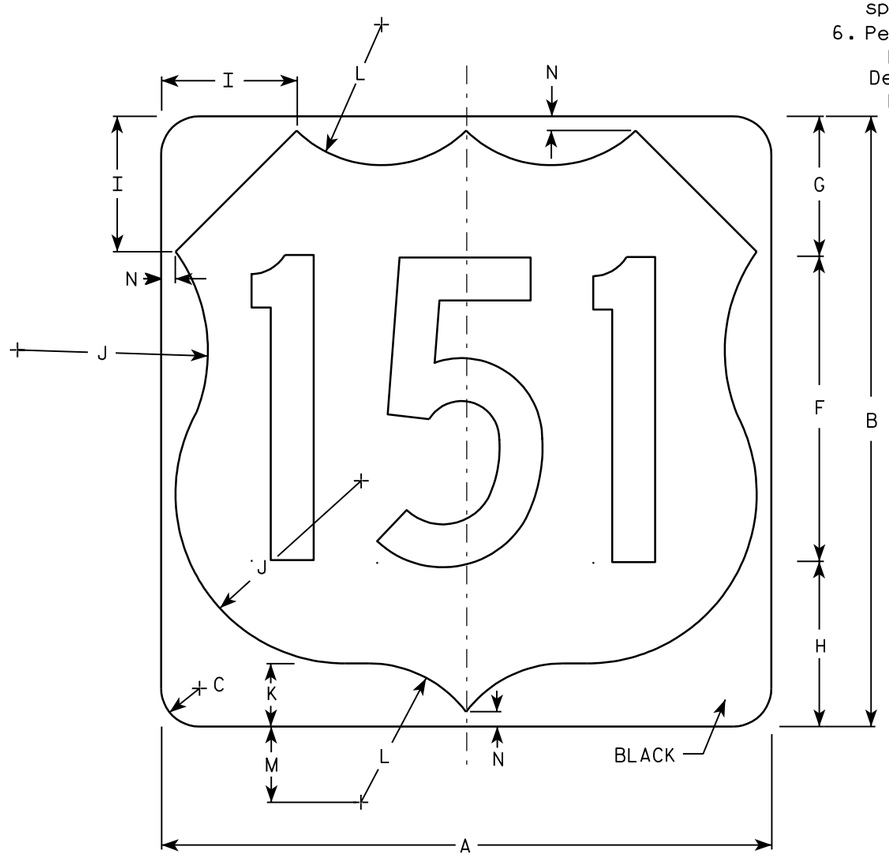
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 97	E

NOTES

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black - See Note 6
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and adjust spacing as per Plate A10-1.
6. Permanent Signs
   Background - Type H Reflective
   Detour or other temporary signs
   Background - Reflective



M1-4



M1-4

Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0	.36
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0	.81

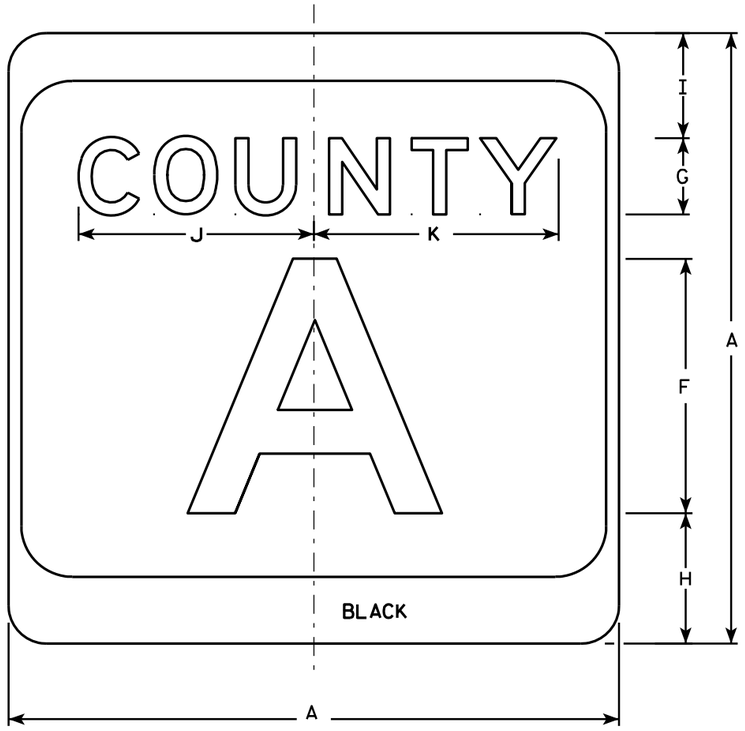
USH MARKER
M1-4 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

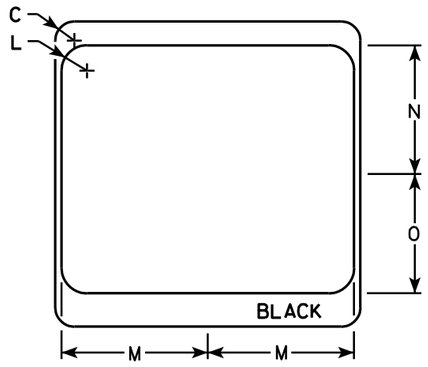
APPROVED Matthew R Rauch
for State Traffic Engineer

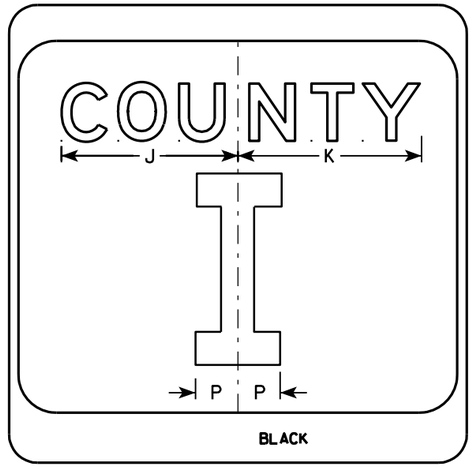
DATE 08/25/05 PLATE NO. M1-4.9

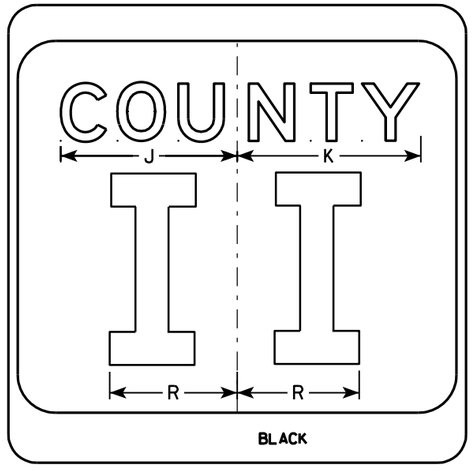
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 98 | E



M1-5A







NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black - See Note 7
   Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
   Message Series D for 2 letters unless message is too big then Series C.
   Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
   Background - Type H Reflective
   Detour or temporary Signs
   Background - Reflective

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

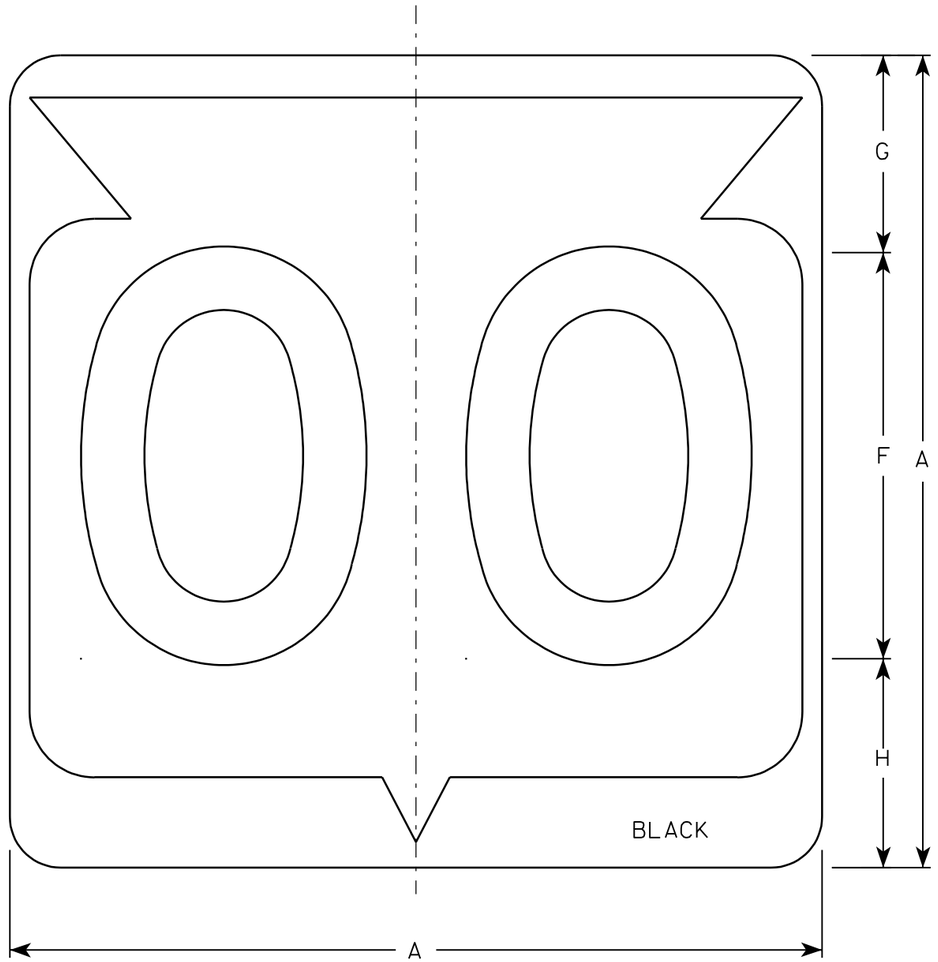
CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/27/11 PLATE NO. M1-5A.8

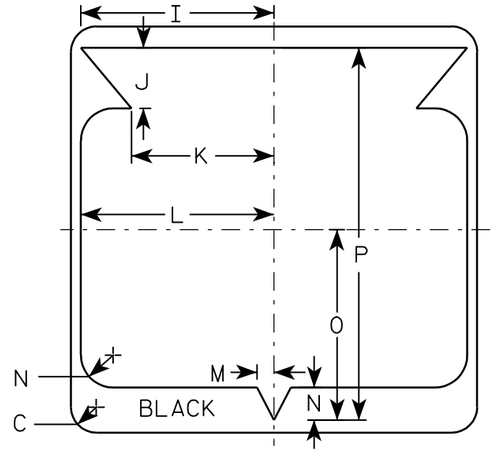
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 99 | E



M1-6

NOTES

1. Sign is Type II - See Note 6 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black - See Note 6
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate Series numerals and adjust spacing as per plate A10-1.
6. Permanent Signs
   Background - Type H Reflective
   Detour or temporary Signs
   Background - Reflective



Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0	.81

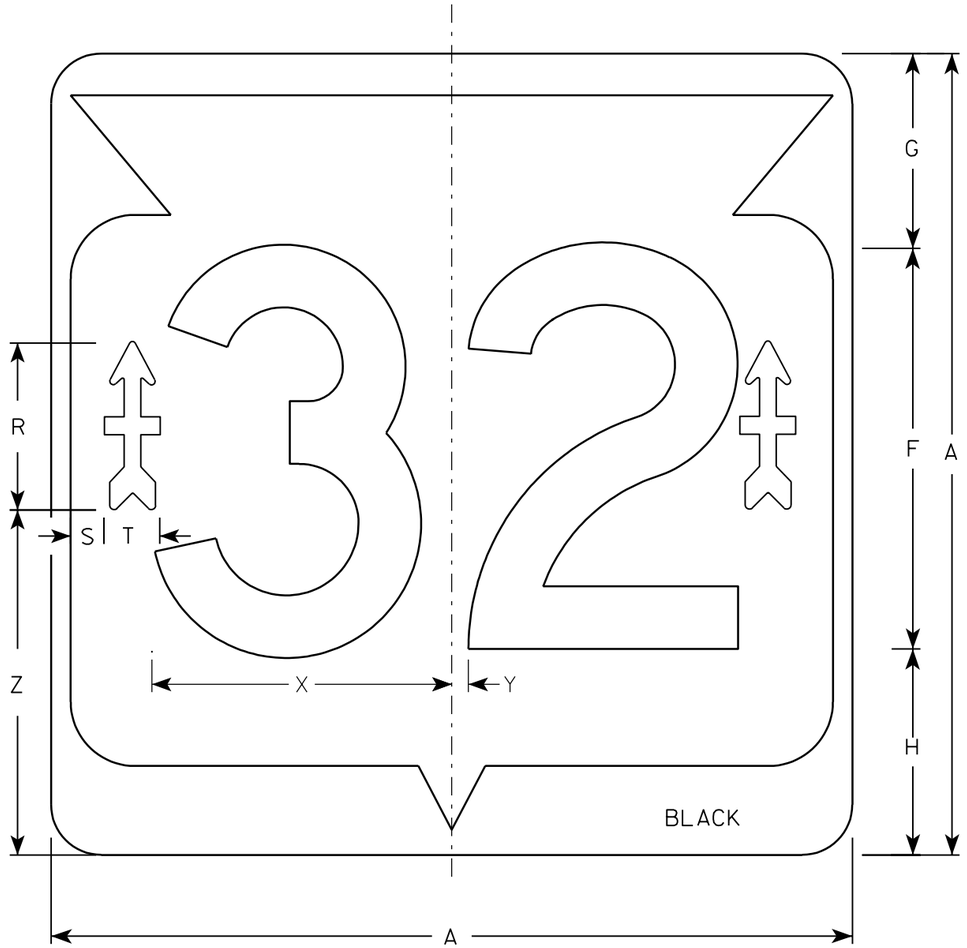
STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Chester J Spang for State Traffic Engineer

DATE 3/20/02 PLATE NO. M1-6.9

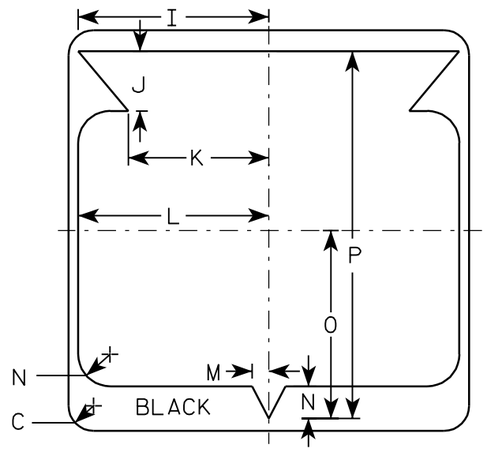
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 100	E



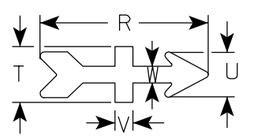
M1-6B

NOTES

1. Sign is Type II - Type H - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black
   Message - Black
   Arrow - Type H Reflective Red
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



32nd DIVISION ARROW
ACTUAL SIZE



Metric equivalent for this sign is:

SIZE	
1	
2	600 mm X 600 mm
3	900 mm X 900 mm
4	900 mm X 900 mm
5	900 mm X 900 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m2
1																												
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8		5 1/8	3/4	1 7/8	1 1/2	5/8	5/8	9	1/2	10 1/2	4.0	.36
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33		7 1/2	1 1/2	2 1/2	2	7/8	3/4	13 1/2	3/4	15 1/2	9.0	.81
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33		7 1/2	1 1/2	2 1/2	2	7/8	3/4	13 1/2	3/4	15 1/2	9.0	.81
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33		7 1/2	1 1/2	2 1/2	2	7/8	3/4	13 1/2	3/4	15 1/2	9.0	.81

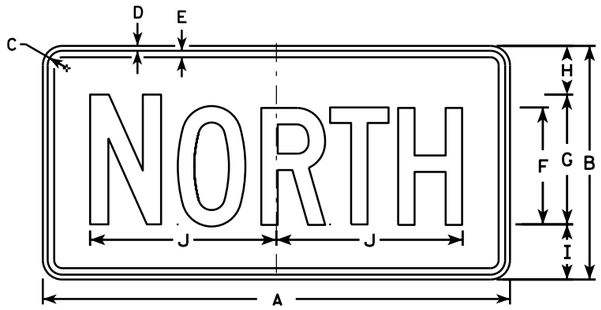
STATE ROUTE MARKER "32"
M1-6B FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

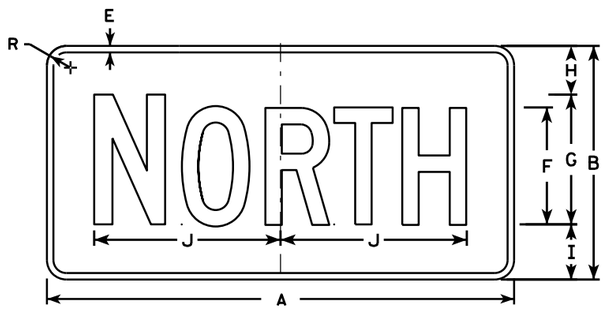
APPROVED Matthew R Rauch for State Traffic Engineer

DATE 12/5/05 PLATE NO. M1-6B.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 101	E

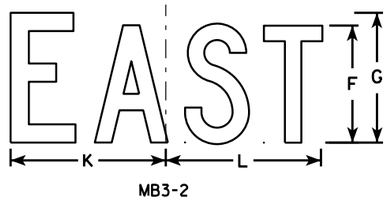


M3-1
MK3-1
MM3-1
MN3-1

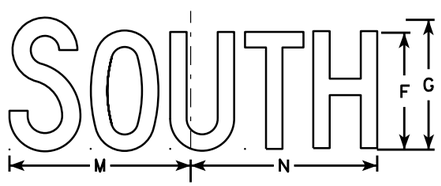


MB3-1

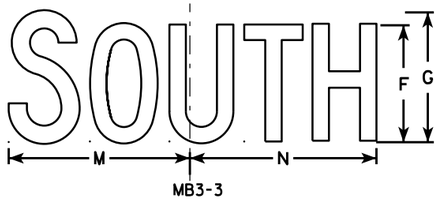
M3-2
MK3-2
MM3-2
MN3-2



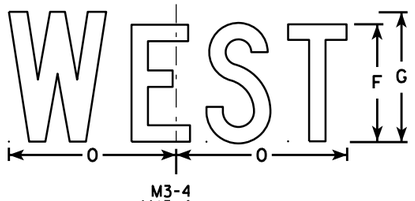
MB3-2



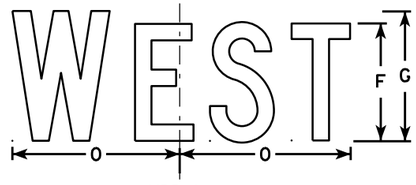
M3-3
MK3-3
MM3-3
MN3-3



MB3-3



M3-4
MK3-4
MM3-4
MN3-4



MB3-4

NOTES

1. All Signs Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
   Message - Black
   MB3-1 thru MB3-4 Background - Blue
   Message - White
   MK3-1 thru MK3-4 Background - Green
   Message - White
   MM3-1 thru MM3-4 Background - White
   Message - Green
   MN3-1 thru MN3-4 Background - Brown
   Message - White
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

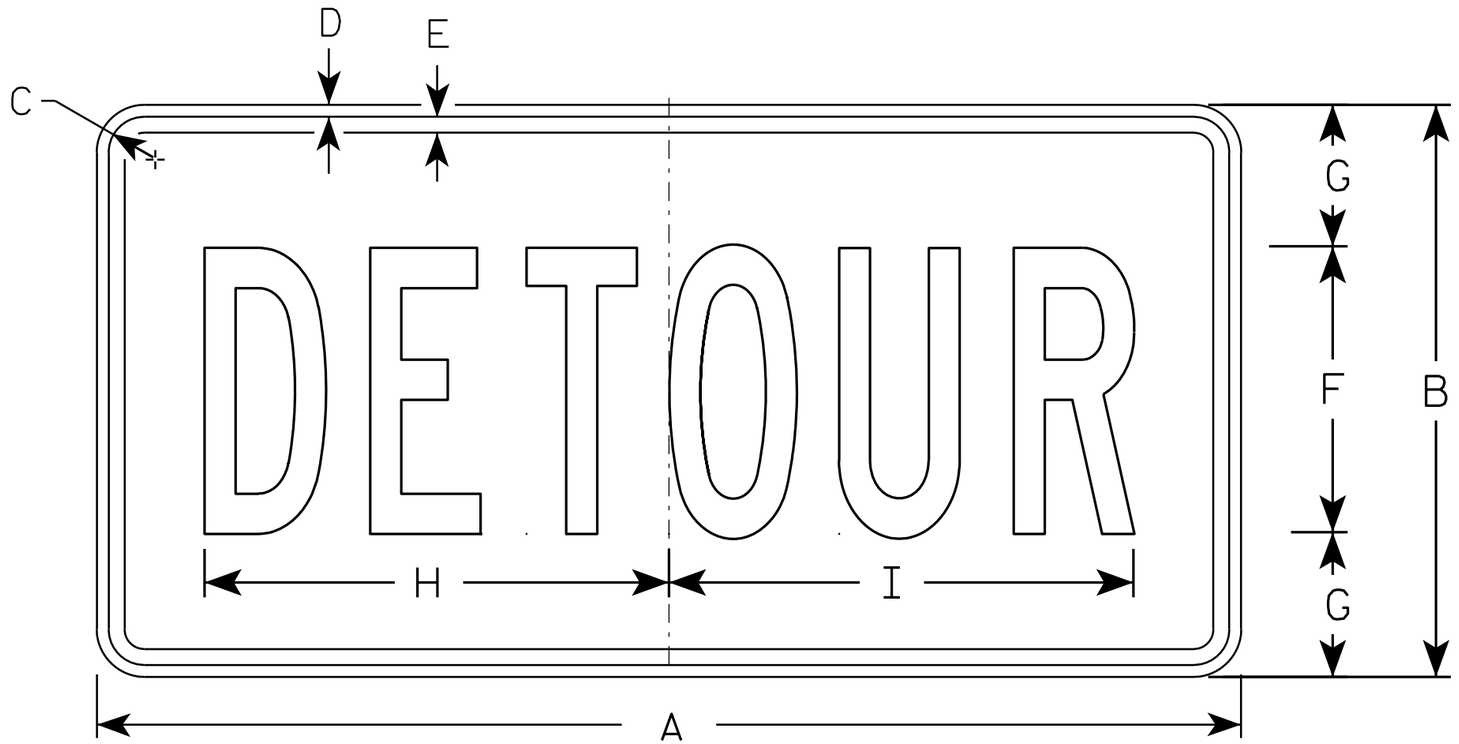
STANDARD SIGNS
M3-1 thur M3-4
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 6/30/14 PLATE NO M3-1.13

PROJECT NO: | HWY: | COUNTY: | | SHEET NO: E



M4-8

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

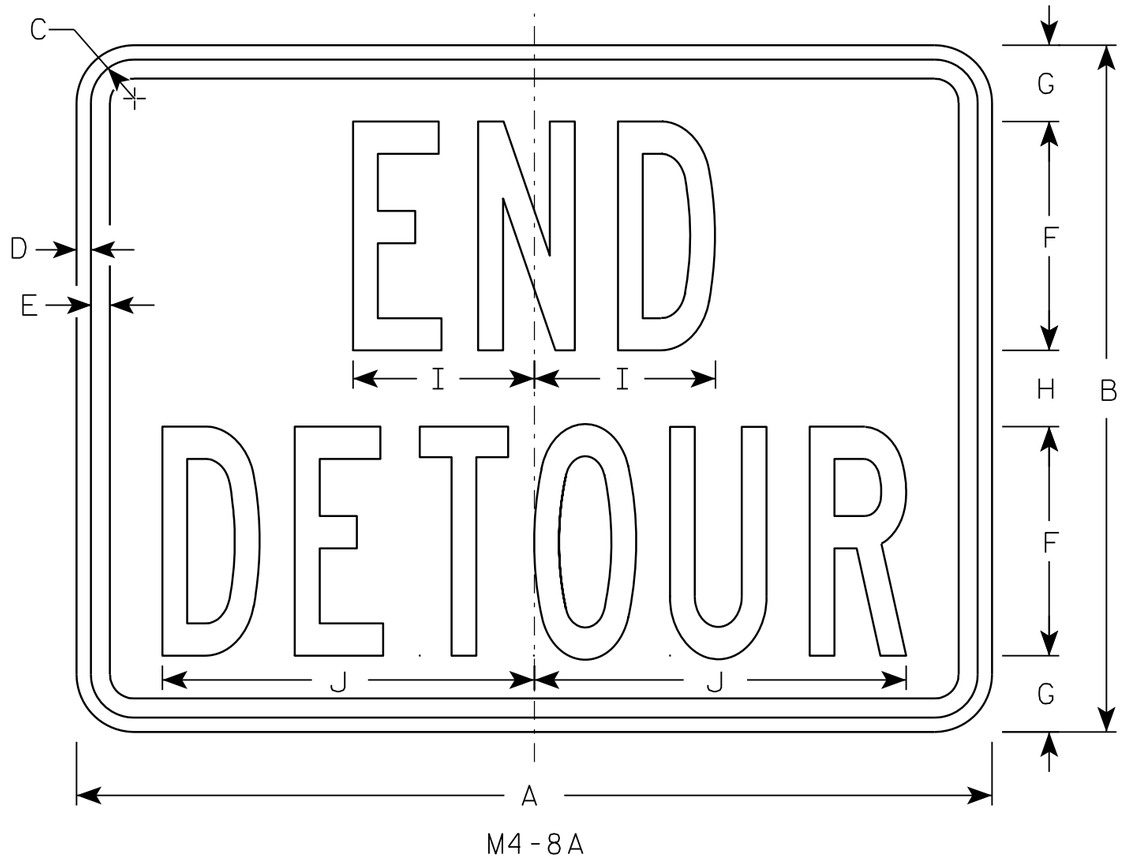
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 103	E



M4-8A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

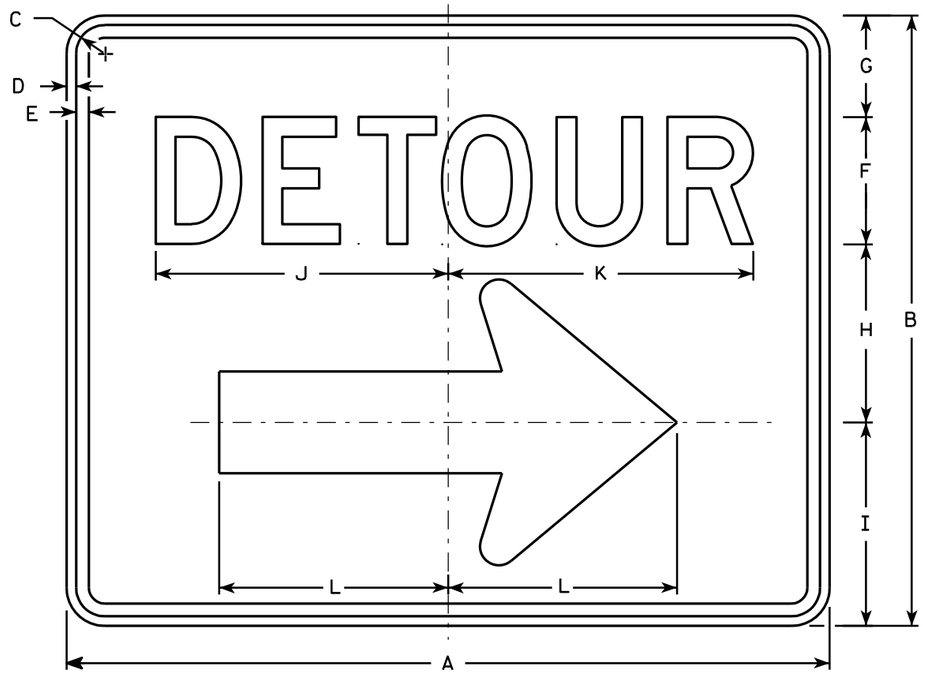
STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

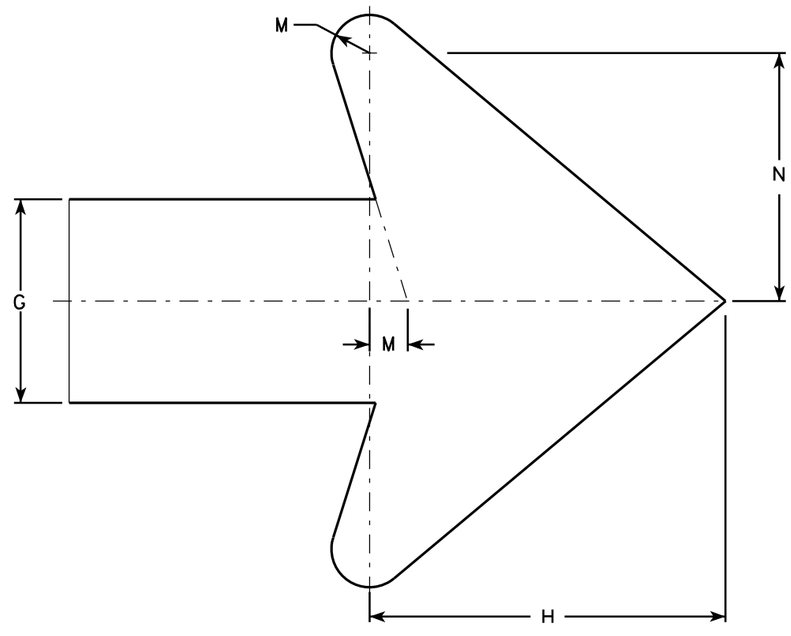
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 104	E



M4-9R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

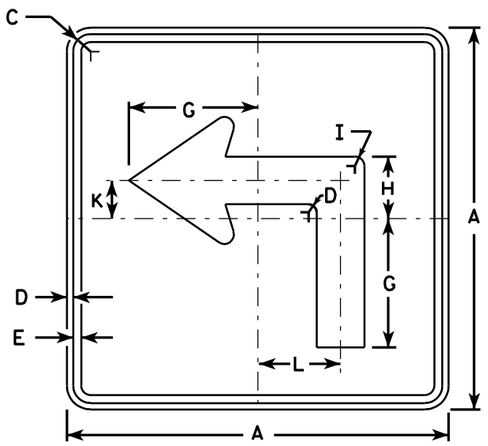
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

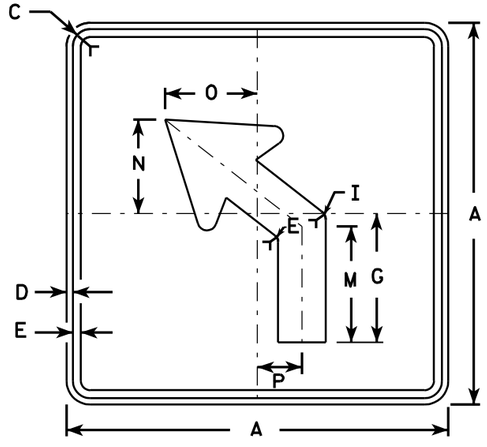
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

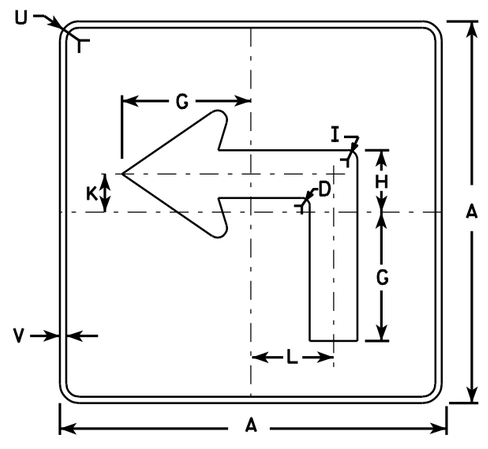
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 105 | E



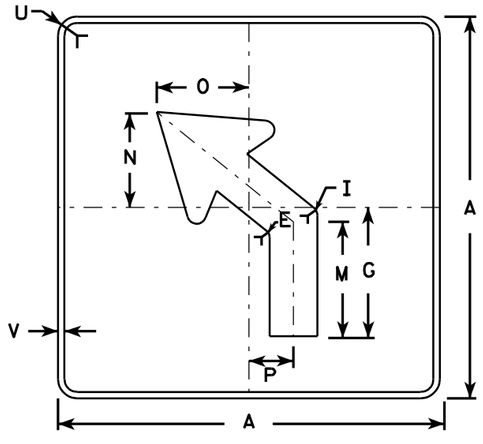
M5-1L
MK5-1L
MM5-1L
MO5-1L
MP5-1L
MR5-1L



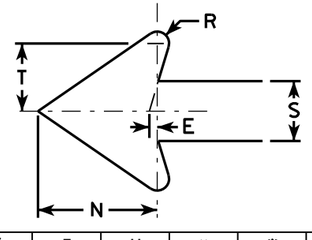
M5-2L
MK5-2L
MM5-2L
MO5-2L
MP5-2L
MR5-2L



MB5-1L
MG5-1L
MN5-1L



MB5-2L
MG5-2L
MN5-2L



NOTES

1. Signs are Type II - See Note 4 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M5-1 and M5-2 Background - White - Type H Reflective
   Message - Black
   MB5-1 and MB5-2 Background - Blue
   Message - White - Type H Reflective
   MG5-1 and MG5-2 Background - Green
   Message - White - Type H Reflective
   MK5-1 and MK5-2 Background - Green
   Message - White Type H Reflective
   MM5-1 and MM5-2 Background - White - Type H Reflective
   Message - Green
   MN5-1 and MN5-2 Background - Brown
   Message - White - Type H Reflective
   MO5-1 and MO5-2 Background - Orange - Type F Reflective
   Message - Black
   MP5-1 and MP5-2 Background - White - Type H Reflective
   Message - Blue
   MR5-1 and MR5-2 Background - Brown
   Message - Yellow - Type H Reflective
5. M5-1R same as M5-1L except arrow points right.
6. M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 1/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 1/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 1/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

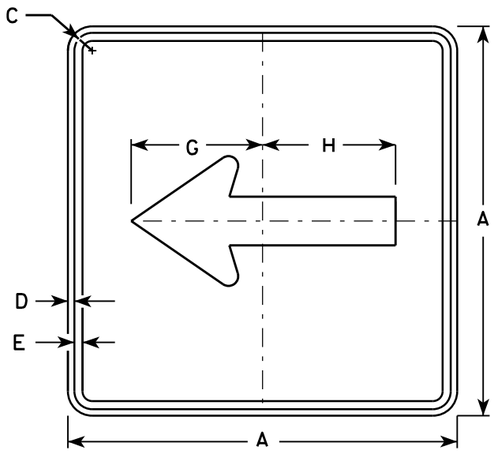
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

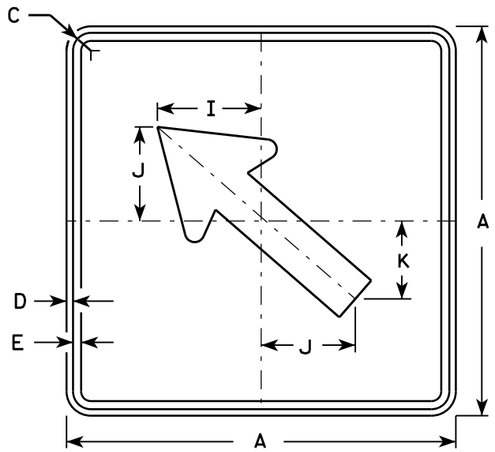
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/29/13 PLATE NO. M5-1.12

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 106 | E



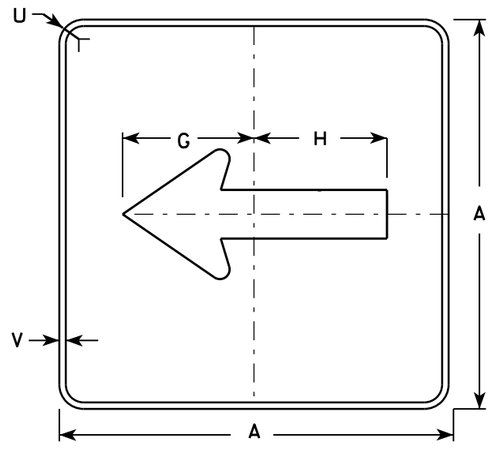
M6-1
MK6-1
MM6-1
MN6-1
MO6-1
MP6-1
MR6-1



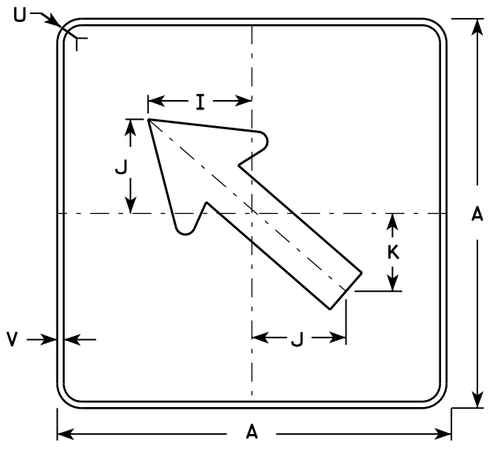
M6-2
MK6-2
MM6-2
MN6-2
MO6-2
MP6-2
MR6-2

NOTES

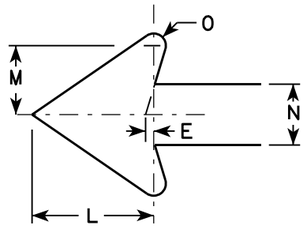
1. Signs are Type II - Type H except as Shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-1 and M6-2 Background - White
   Message - Black
   MB6-1 and MB6-2 Background - Blue
   Message - White
   MG6-1 and MG6-2 Background - Green
   Message - White
   MK6-1 and MK6-2 Background - Green
   Message - White
   MM6-1 and MM6-2 Background - White
   Message - Green
   MN6-1 and MN6-2 Background - Brown
   Message - White
   MO6-1 and MO6-2 Background - Orange - Type F Reflective
   Message - Black
   MP6-1 and MP6-2 Background - White
   Message - Blue
   MR6-1 and MR6-2 Background - Brown
   Message - Yellow



MB6-1



MB6-2



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2						3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25

STANDARD SIGN M6-1 & M6-2 SERIES

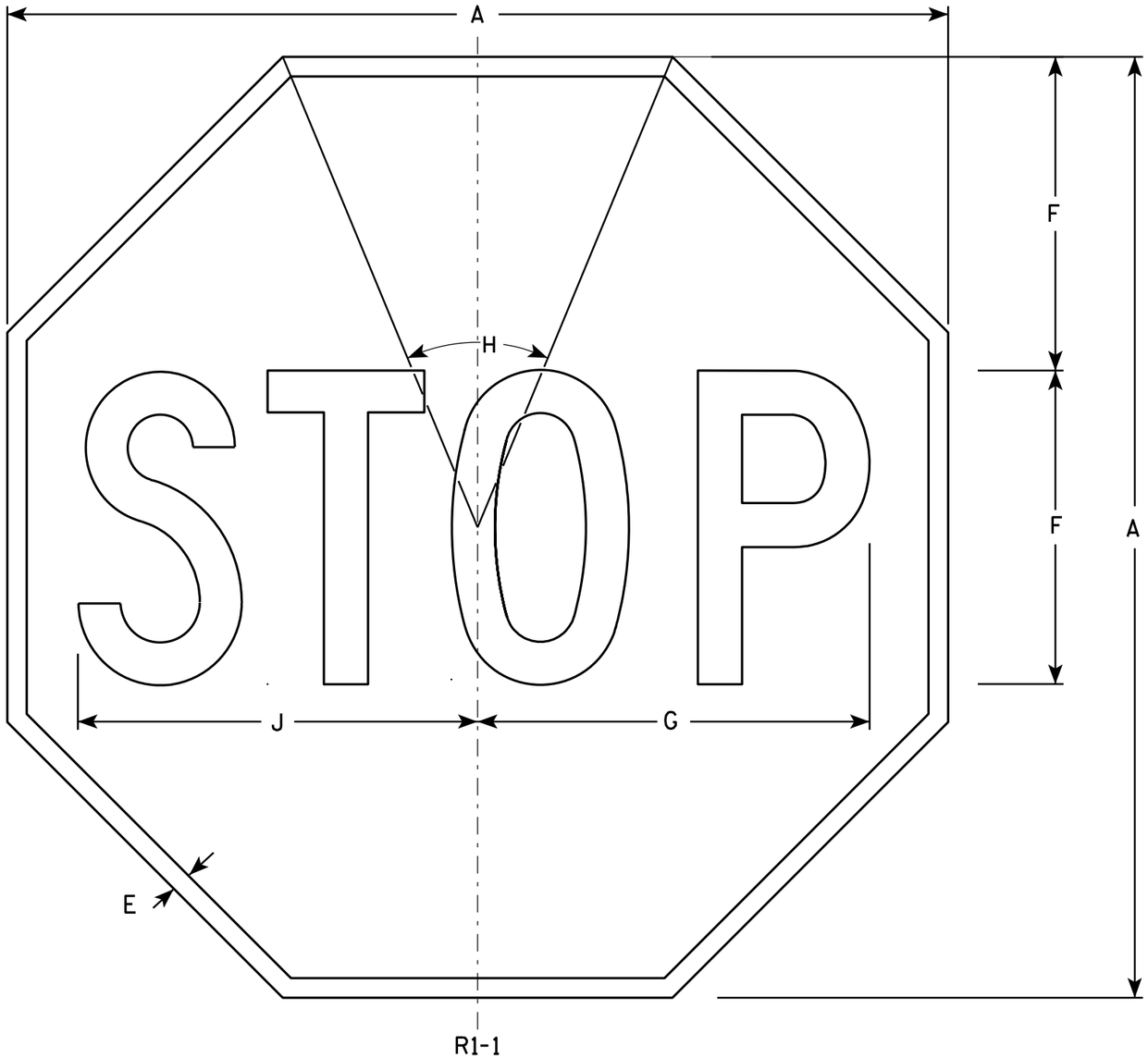
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 7/03/14 PLATE M6-1.14

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\M61.DGN PLOT DATE : 03-JUL-2014 14:28 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 11.675051:1.000000
WISDOT/CADDS SHEET 42



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Red
   Message - White
3. Message Series - C

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

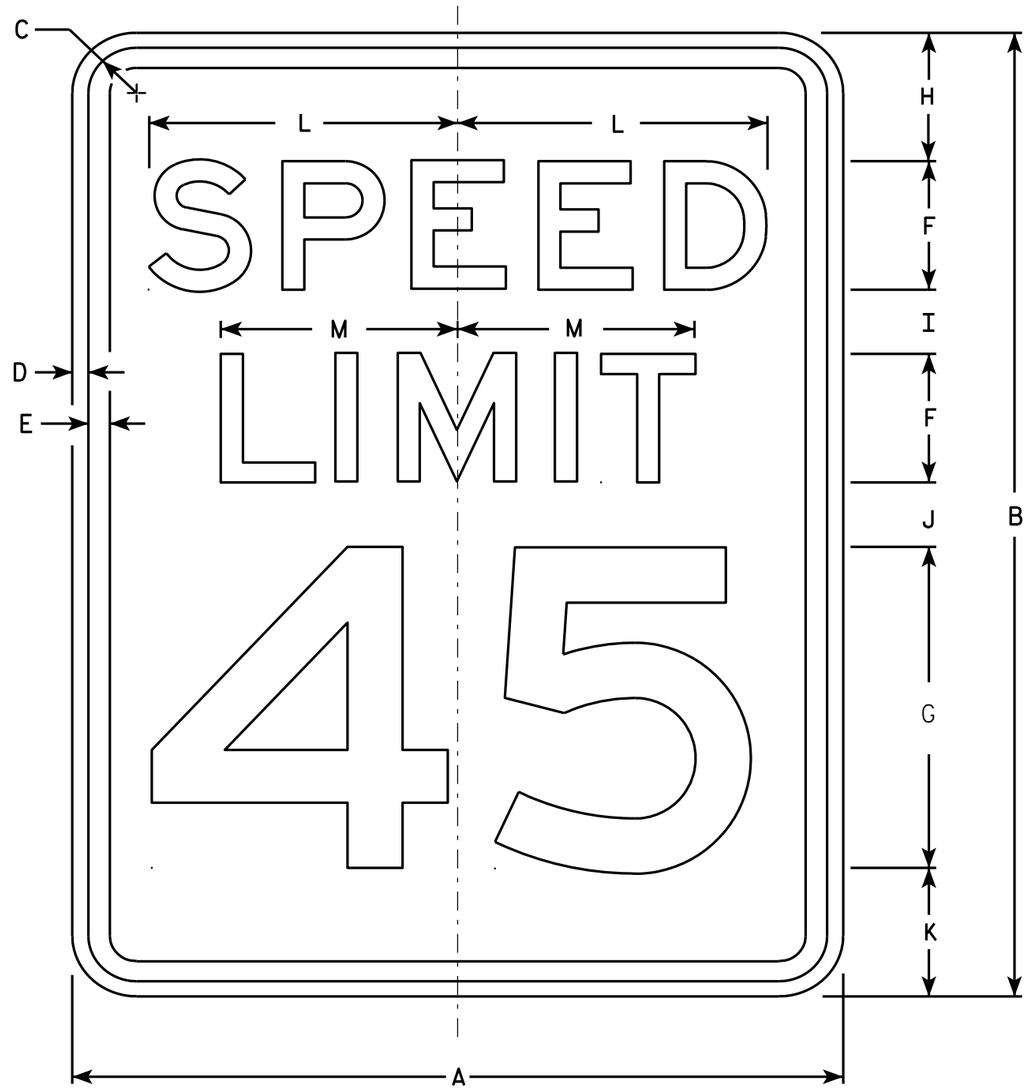
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 108	E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

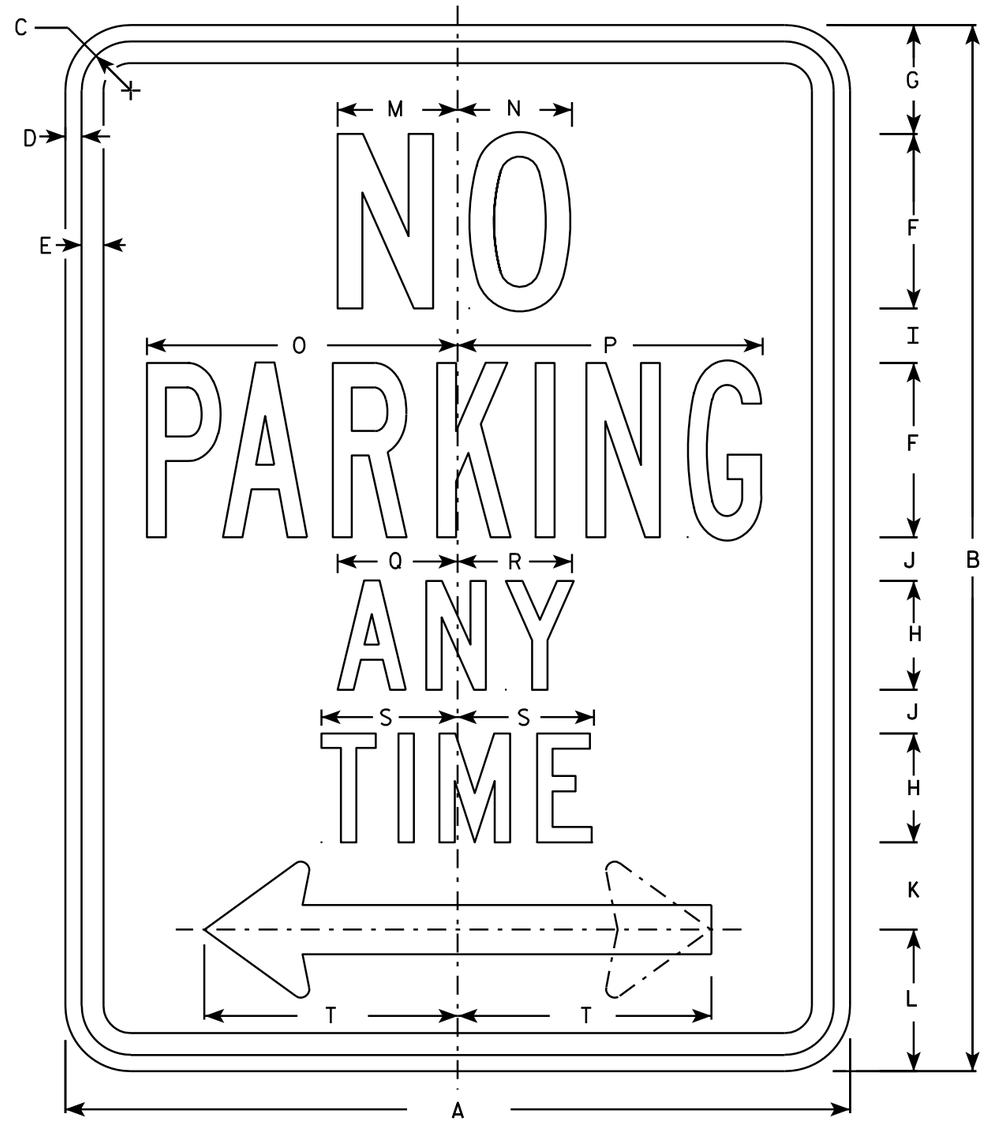
STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

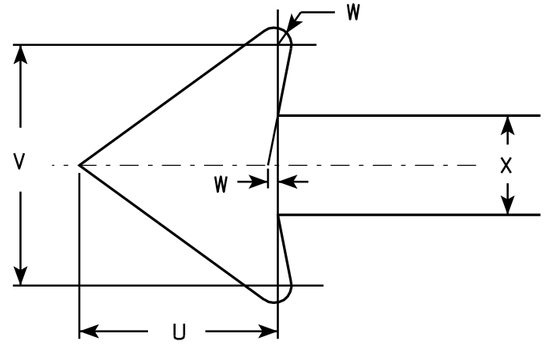
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 109	E



R7-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Red
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 3 and 4 are series C, line 2 is series B.
6. R7-1D (double arrow)
   R7-1L (left arrow)
   R7-1R (right arrow)



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/8	3/8	3/8	3	1 7/8	2	7/8	5/8	1 1/2	2 1/2	2	2	4 7/8	4 7/8	2 1/4	2 1/8	2 1/2	3 7/8	1 1/2	1 3/4	1/8	3/4			1.5
2S	18	24	1 1/8	3/8	1/2	4	2 1/2	2 1/2	1 1/4	1	2	3 1/4	2 3/4	2 5/8	7 1/8	7	2 3/4	2 5/8	3 1/8	5 7/8	2 1/4	2 5/8	1/4	1 1/8			3.0
2M	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
3	24	30	1 1/8	3/8	1/2	5	3	3	2	1 1/4	2 1/2	4	3 1/4	3 3/8	9 1/4	9 1/4	3 1/4	3 1/4	3 3/4	7 3/4	3	3 1/2	1/4	1 1/2			5.0
4																											
5																											

STANDARD SIGN
R7-1

WISCONSIN DEPT OF TRANSPORTATION

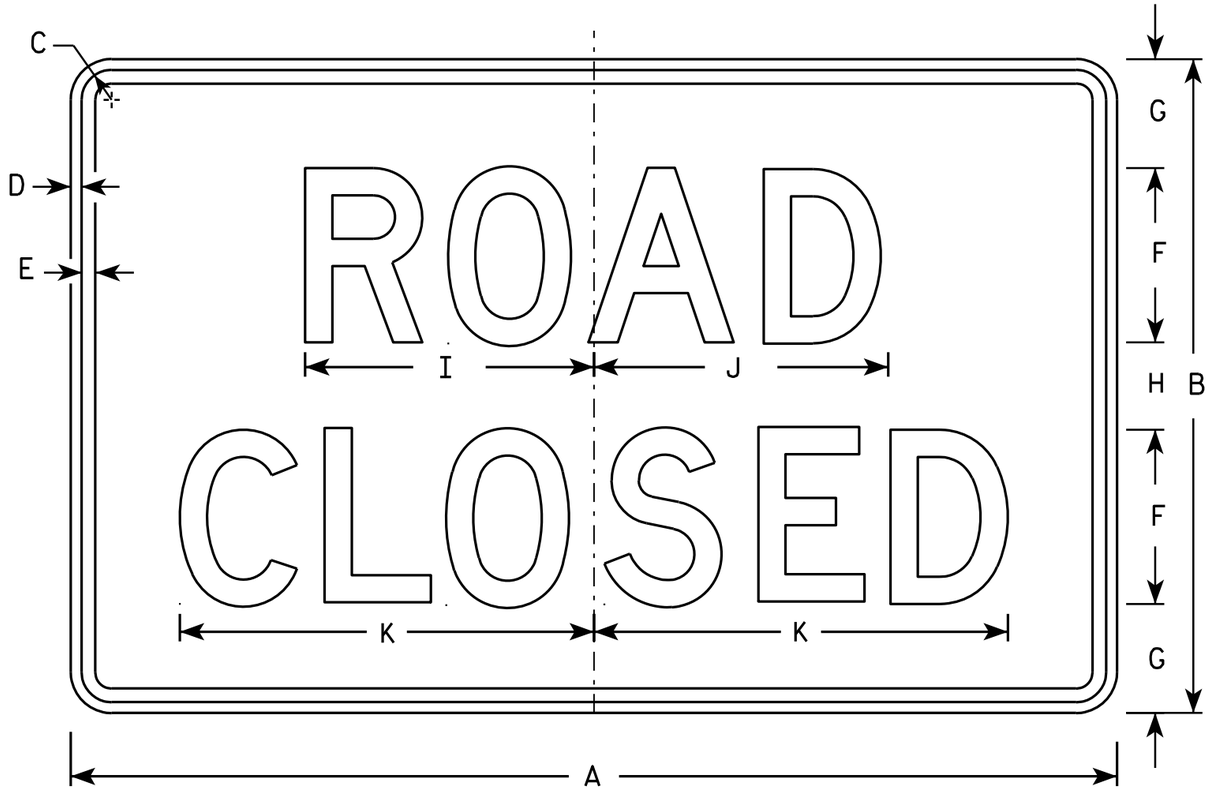
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/31/2011 PLATE NO. R7-1.9

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 110 | E

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R71.DGN PLOT DATE : 31-MAR-2011 09:20 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 3.476110:1.000000

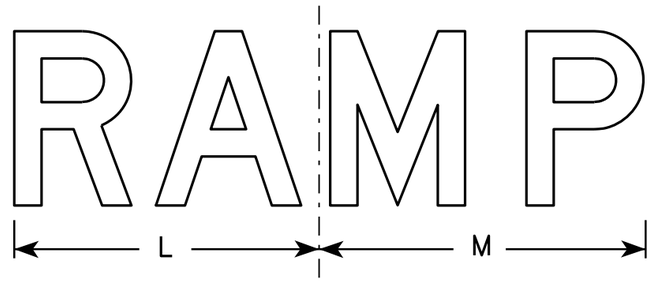
WISDOT/CADDS SHEET 42



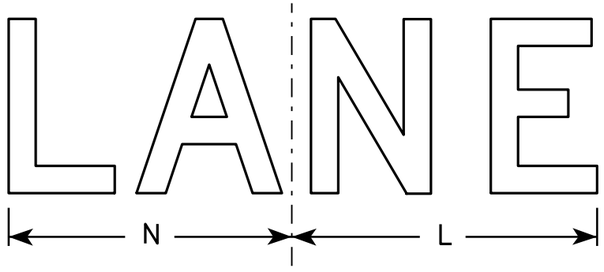
R11-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.



R11-2R



R11-2L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13													10.0

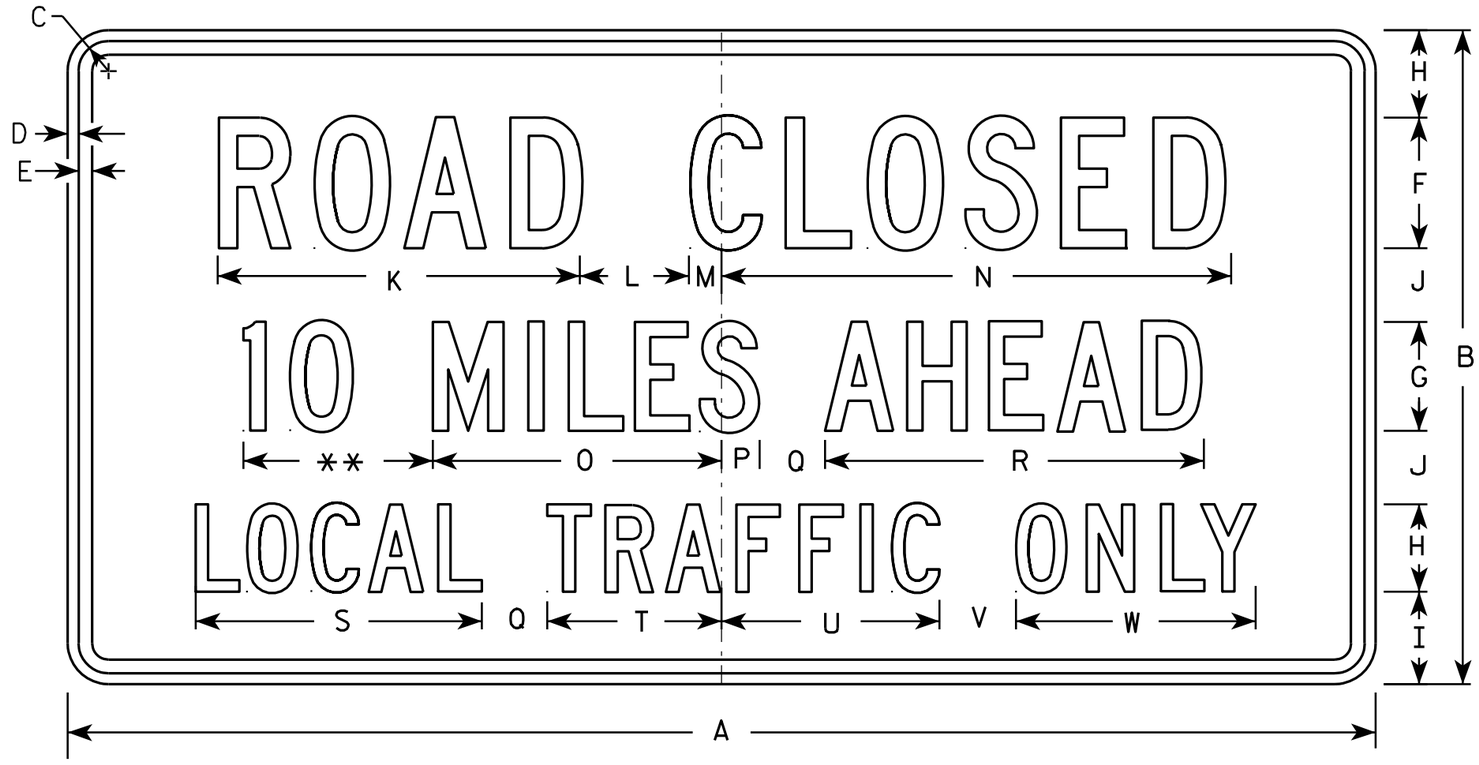
STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-2.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 111 | E



R11-3

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

** See Note 5

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 3/8	1/2	5/8	4	3	2 1/2	2	2	11 1/8	3	1 1/8	15 1/4	8	1 1/2	2	10 3/4	8 3/8	4 3/4	6 1/2	2	6 3/4				4.5
2S	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	16 5/8	5	1 1/2	23	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11				12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	4 1/4	3 3/8	16 5/8	5	1 1/2	23	13 1/4	1 3/4	3	17 3/8	13 1/8	8	10	3 1/2	11				12.5
3																											
4																											
5																											

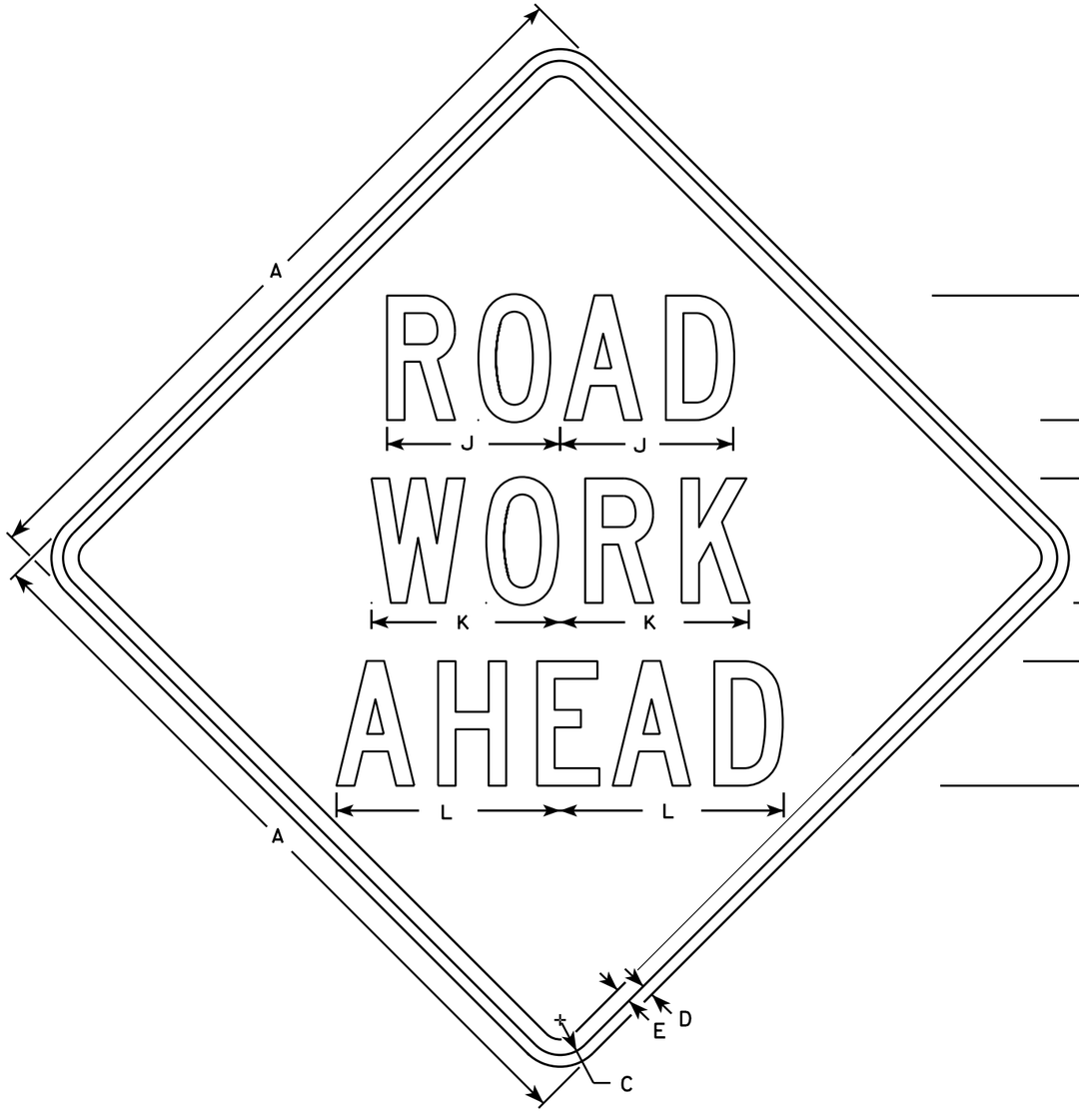
STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

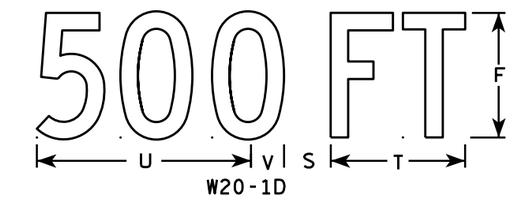
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-3.6

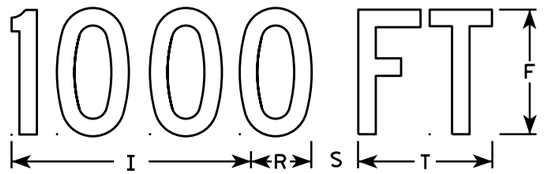
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 112 | E



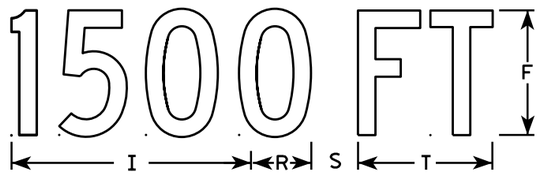
W20-1A



W20-1D



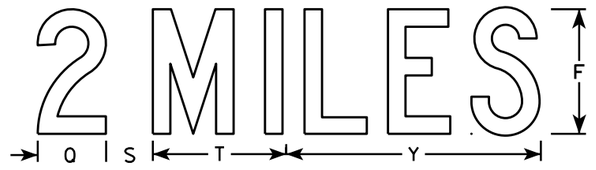
W20-1C



W20-1B

W20-1G

W20-1F



W20-1E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 3/8	1/2	5/8	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9		2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

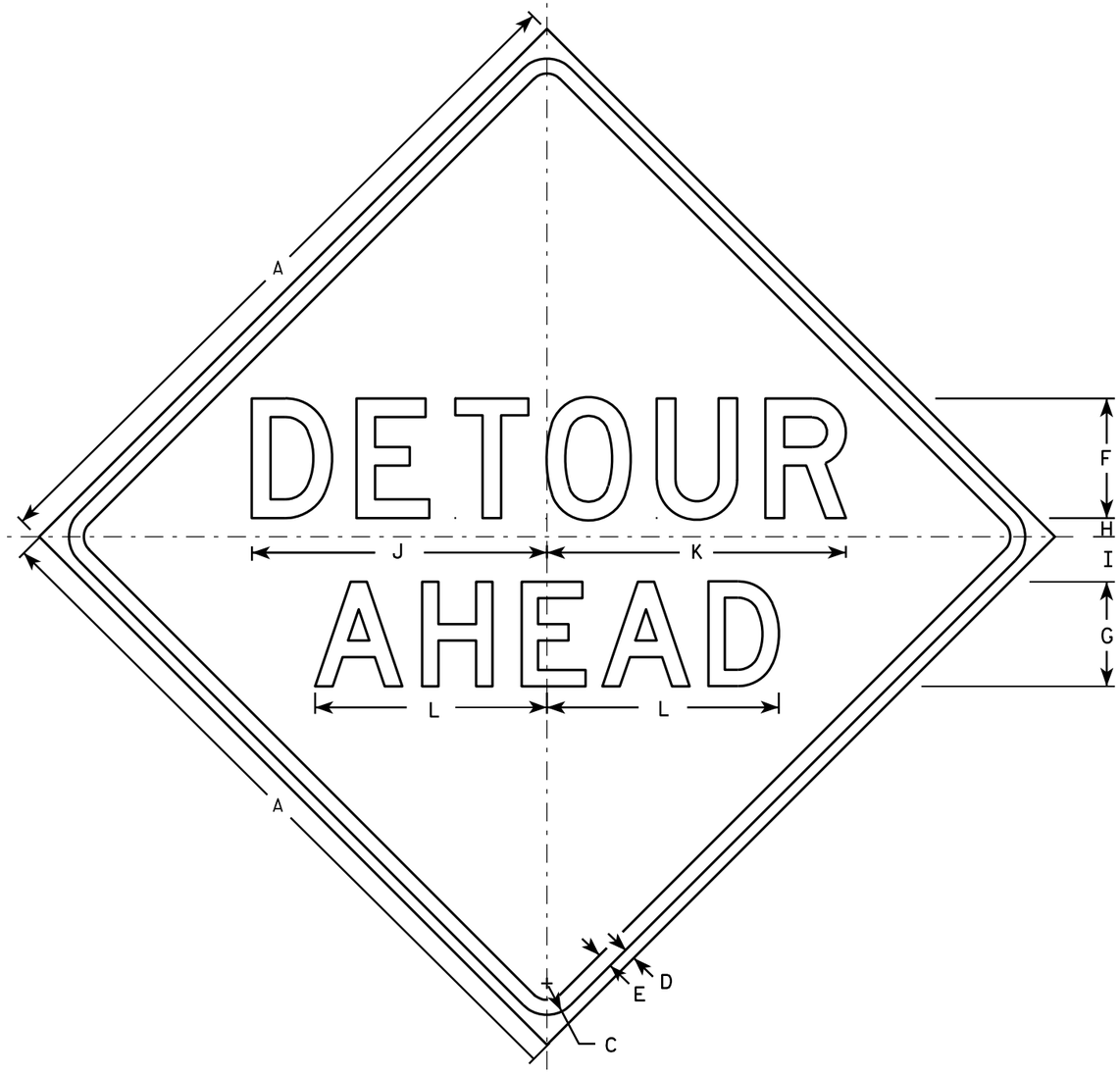
State Traffic Engineer

DATE 5/07/15 PLATE NO. W20-1.10

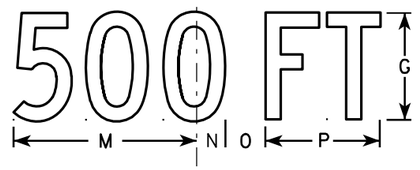
PROJECT NO: | SHEET NO: E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W201.dgn PLOT DATE : 27-MAY-2015 15:58 PLOT BY : mscsja

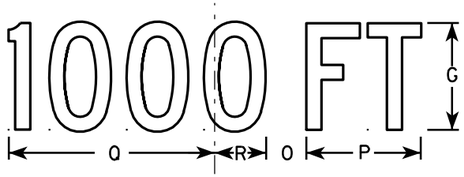
WISDOT/CADDS SHEET 42



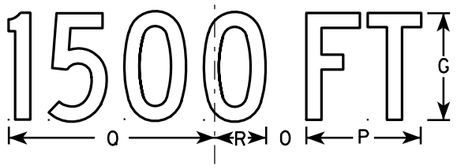
W20-2A



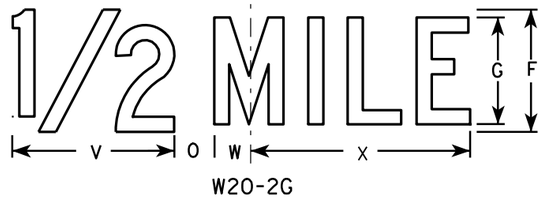
W20-2D



W20-2C



W20-2B



W20-2G

W20-2F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
   Line 2 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

STANDARD SIGN
W20-2A,B,C,D,F & G

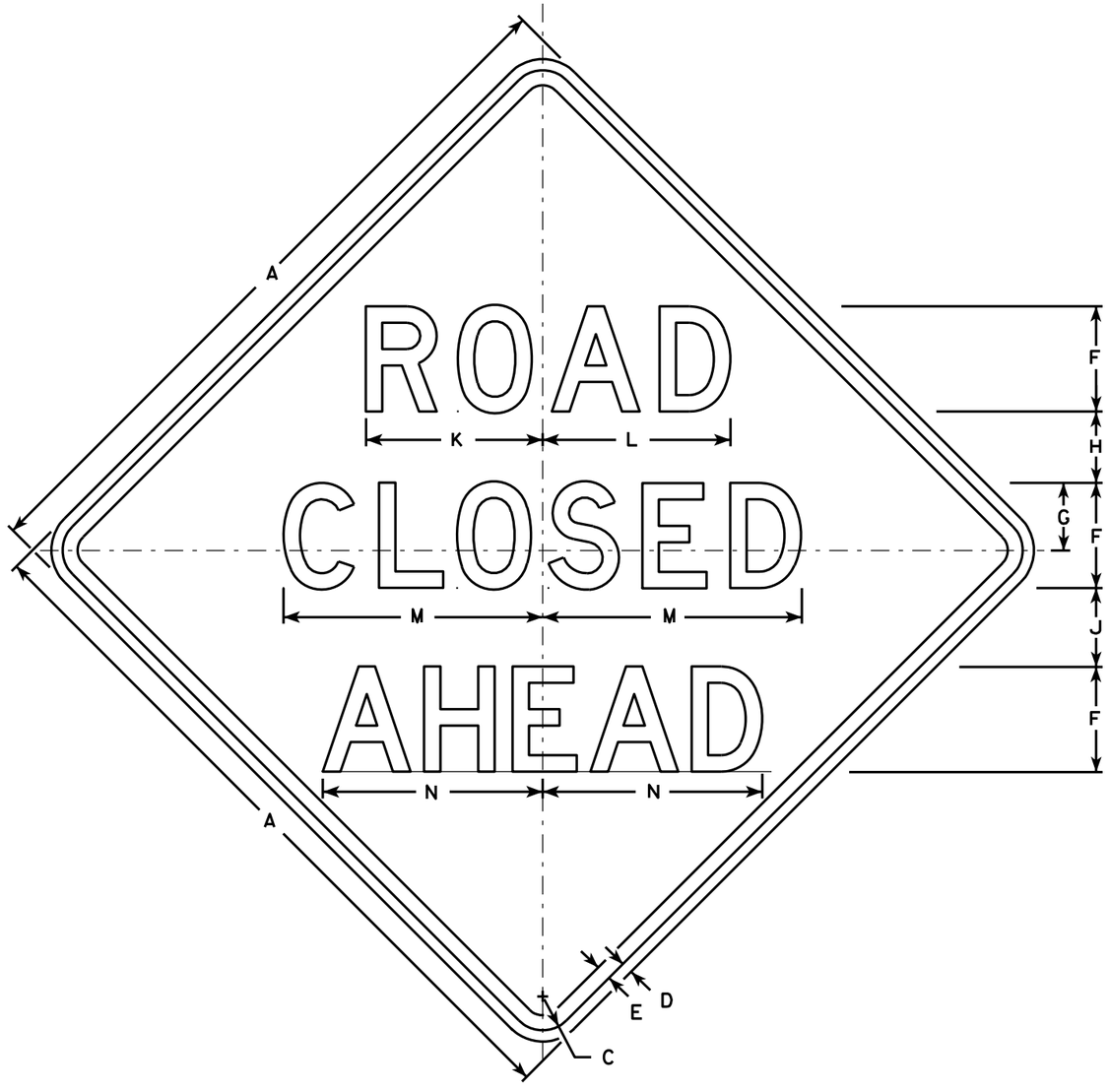
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

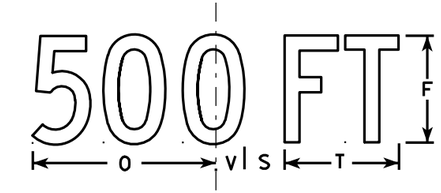
DATE 3/18/11 PLATE NO. W20-2.6

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 114 | E

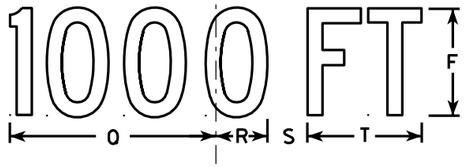
FILE NAME : C:\Users\PROJECTS\tr_stdplate\W202.DGN PLOT DATE : 18-MAR-2011 10:00 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42



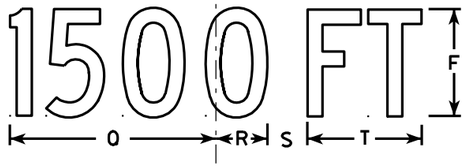
W20-3A



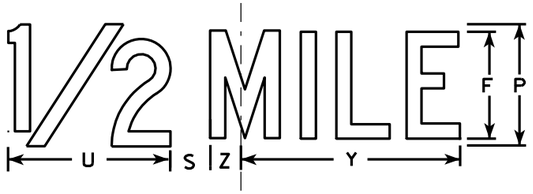
W20-3D



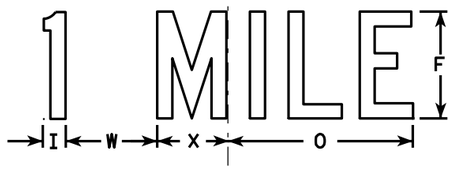
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
   Line 3 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

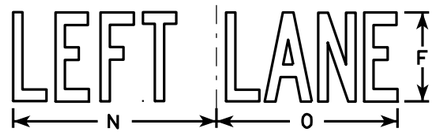
APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 3/18/11 PLATE NO. W20-3.7

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 115 | E

7

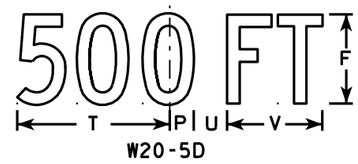
FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN PLOT DATE : 18-MAR-2011 12:08 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42

W20-56A

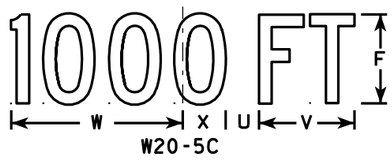


W20-55A

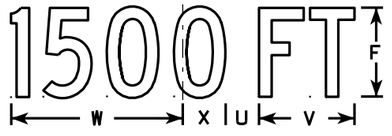
W20-5A



W20-5D



W20-5C



W20-5B

W20-5G

W20-5F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. " _____ LANE" is Series B.
   All other copy is Series C.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	1/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 1/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

STANDARD SIGN
W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

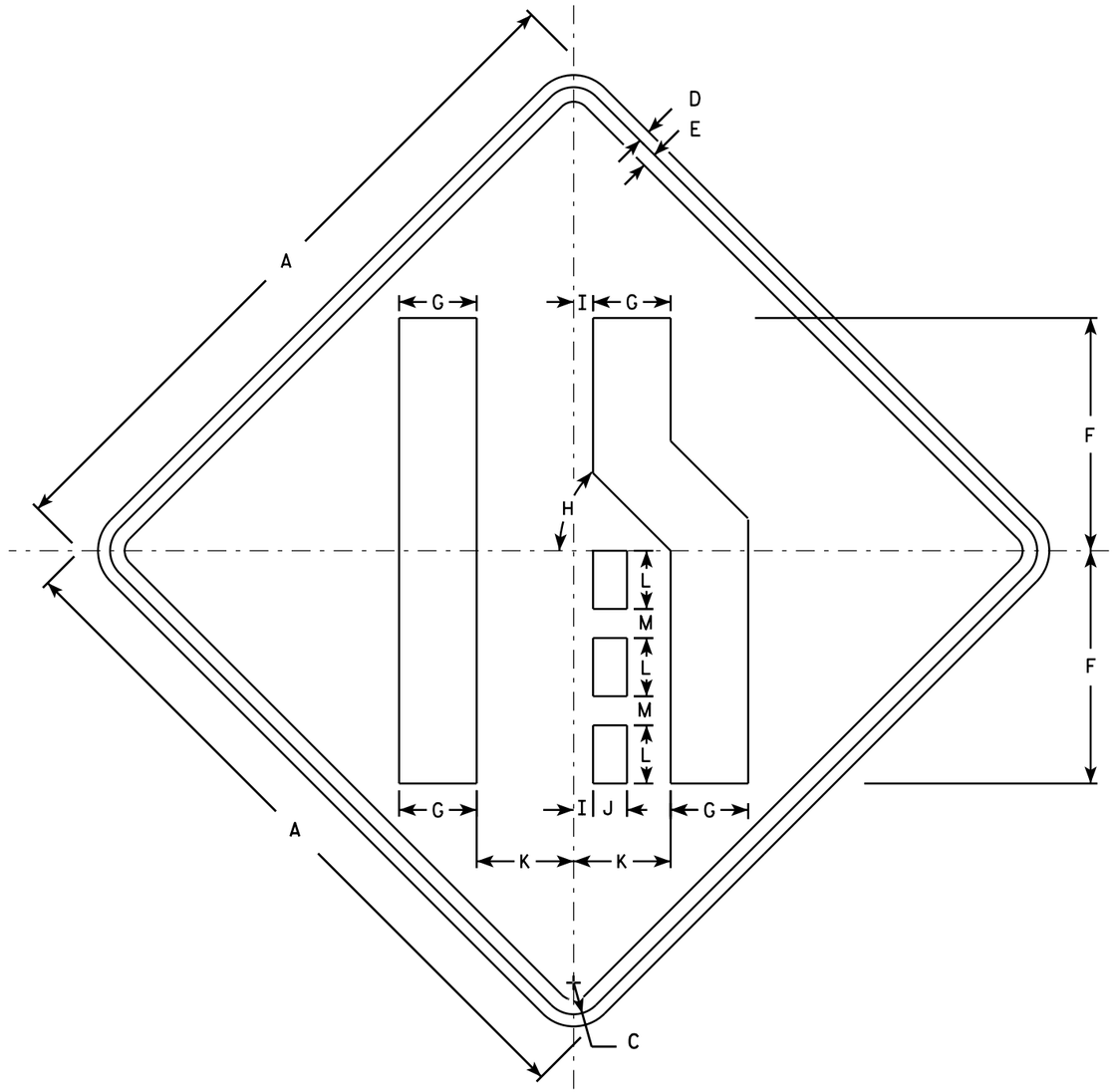
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-5.11

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 116 | E

7

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W205.DGN PLOT DATE : 18-MAR-2011 12:15 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 11.918087:1.000000
WISDOT/CADDS SHEET 42



W04-2R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W04-2L is the same as W04-2R except the symbol is reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	12	4	45°	1	1 3/4	5	3	1 1/2														9.0
2S	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
2M	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
3	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
4	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0
5	48		2 1/4	3/4	1	16	5 3/8	45°	1 1/4	2 3/8	6 3/4	4	2														16.0

STANDARD SIGN
W04-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/20/13 PLATE 117 W04-2.1

PROJECT NO:

SHEET NO: E

USH 151 (E. CHESTNUT STREET) EARTHWORK

STATION	Real Station	Distance	AREA (SF)				Incremental Vol (CY) (Unadjusted)				Cumulative Vol (CY)		Mass Ordinate
			Cut	Salvaged / Unusable Pavement Material	Fill	EBS	Cut	Salvaged / Unusable Pavement Material	Fill	EBS	Cut 1.00	Expanded Fill 1.33	
211+13.94	21113.94	0.00	127.91	48.33	0.70	0.00	0	0	0	0	0	0	0.00
211+25	21125.00	11.06	128.02	48.33	0.24	0.00	52	20	0	0	52	0	32.37
211+50	21150.00	25.00	130.35	48.33	0.69	0.00	120	45	0	0	172	1	106.66
211+75	21175.00	25.00	137.43	48.33	0.20	0.00	124	45	0	0	296	1	185.34
212+00	21200.00	25.00	145.76	48.33	0.00	0.00	131	45	0	0	427	1	271.57
212+25	21225.00	25.00	143.19	48.33	0.01	0.00	134	45	0	0	561	2	360.58
212+50	21250.00	25.00	135.79	48.33	0.11	0.00	129	45	0	0	690	2	444.91
212+75	21275.00	25.00	135.05	48.33	0.15	0.00	125	45	0	0	815	2	525.39
213+00	21300.00	25.00	138.66	48.33	0.23	0.00	127	45	0	0	942	2	607.12
213+20.54	21320.54	20.54	135.68	48.33	0.18	0.00	104	37	0	0	1,047	2	674.49
213+25	21325.00	4.46	135.29	48.33	0.33	0.00	22	8	0	0	1,069	2	688.84
213+30	21330.00	5.00	138.23	48.33	0.34	0.00	25	9	0	0	1,094	2	705.13
213+35	21335.00	5.00	130.15	48.33	0.30	0.00	25	9	0	0	1,119	2	720.95
213+40	21340.00	5.00	139.72	48.33	0.82	0.00	25	9	0	0	1,144	3	736.85
213+45	21345.00	5.00	145.34	48.33	0.32	0.00	26	9	0	0	1,170	3	754.16
213+50	21350.00	5.00	141.88	48.33	0.00	0.00	27	9	0	0	1,197	3	771.76
213+55	21355.00	5.00	130.90	48.33	0.00	0.00	25	9	0	0	1,222	3	788.07
213+60	21360.00	5.00	120.21	48.33	0.00	0.00	23	9	0	0	1,246	3	802.37
213+65	21365.00	5.00	113.89	48.33	0.00	0.00	22	9	0	0	1,267	3	815.09
213+66.46	21366.46	1.46	114.49	48.33	0.00	0.00	6	3	0	0	1,273	3	818.65
213+70	21370.00	3.54	117.63	48.33	0.00	0.00	15	6	0	0	1,289	3	827.53
213+75	21375.00	5.00	127.28	48.33	0.00	0.00	23	9	0	0	1,311	3	841.26
213+80	21380.00	5.00	138.00	48.33	0.00	0.00	25	9	0	0	1,336	3	856.87
213+85	21385.00	5.00	147.95	48.33	0.00	0.00	26	9	0	0	1,362	3	874.40
213+90	21390.00	5.00	135.75	48.33	0.47	0.00	26	9	0	0	1,389	3	891.66
213+95	21395.00	5.00	132.58	48.33	0.15	0.00	25	9	0	0	1,413	3	907.48
214+00	21400.00	5.00	130.70	48.33	0.57	0.00	24	9	0	0	1,438	3	922.81
214+05	21405.00	5.00	133.12	48.33	0.54	0.00	24	9	0	0	1,462	3	938.15
214+10	21410.00	5.00	130.01	48.33	0.67	0.00	24	9	0	0	1,487	3	953.42
214+15	21415.00	5.00	131.53	48.33	0.64	0.00	24	9	0	0	1,511	3	968.52
214+15.83	21415.83	0.83	131.41	48.33	0.54	0.00	4	1	0	0	1,515	3	971.04
214+25	21425.00	9.17	133.17	48.33	0.01	0.00	45	16	0	0	1,560	4	999.45
214+50	21450.00	25.00	132.24	48.33	0.04	0.00	123	45	0	0	1,683	4	1077.54
214+75	21475.00	25.00	128.68	48.33	1.28	0.00	121	45	1	0	1,803	4	1152.77
215+00	21500.00	25.00	124.02	48.33	1.22	0.00	117	45	1	0	1,920	6	1223.47
215+25	21525.00	25.00	121.62	48.33	0.73	0.00	114	45	1	0	2,034	7	1291.24
215+50	21550.00	25.00	115.51	48.33	2.88	0.00	110	45	2	0	2,144	9	1354.05
215+75	21575.00	25.00	120.39	48.33	0.25	0.00	109	45	1	0	2,253	11	1416.58
216+00	21600.00	25.00	122.15	48.33	1.58	0.00	112	45	1	0	2,365	12	1482.99
216+25	21625.00	25.00	117.76	48.33	1.24	0.00	111	45	1	0	2,477	14	1547.57
216+50	21650.00	25.00	128.18	48.33	0.55	0.00	114	45	1	0	2,590	15	1615.57
216+75	21675.00	25.00	136.38	48.33	0.77	0.00	122	45	1	0	2,713	16	1692.49
217+00	21700.00	25.00	146.00	48.33	0.00	0.00	131	45	0	0	2,844	17	1778.00
217+25	21725.00	25.00	144.67	48.33	0.00	0.00	135	45	0	0	2,978	17	1867.81
217+50	21750.00	25.00	140.74	48.33	0.16	0.00	132	45	0	0	3,110	17	1955.10
217+75	21775.00	25.00	143.36	48.33	0.33	0.00	132	45	0	0	3,242	17	2041.58
217+89.09	21789.09	14.09	144.01	48.33	0.46	0.00	75	25	0	0	3,317	17	2091.08
217+90	21790.00	0.91	144.13	48.33	0.46	0.00	5	2	0	0	3,322	17	2094.28
217+95	21795.00	5.00	140.41	48.33	0.71	0.00	26	9	0	0	3,348	17	2111.53
218+00	21800.00	5.00	144.08	48.33	0.42	0.00	26	9	0	0	3,374	17	2128.78
218+05	21805.00	5.00	140.62	48.33	0.34	0.00	26	9	0	0	3,401	18	2146.10
218+10	21810.00	5.00	139.28	48.33	0.53	0.00	26	9	0	0	3,427	18	2162.96
218+15	21815.00	5.00	150.62	48.33	0.11	0.00	27	9	0	0	3,454	18	2180.77
218+20	21820.00	5.00	145.75	48.33	0.00	0.00	27	9	0	0	3,481	18	2199.25
218+25	21825.00	5.00	137.45	48.33	0.00	0.00	26	9	0	0	3,507	18	2216.52
218+30	21830.00	5.00	129.48	48.33	0.00	0.00	25	9	0	0	3,532	18	2232.29
218+35	21835.00	5.00	132.07	48.33	0.00	0.00	24	9	0	0	3,556	18	2247.55
218+40	21840.00	5.00	137.99	48.33	0.00	0.00	25	9	0	0	3,581	18	2263.61
218+45	21845.00	5.00	142.08	48.33	0.01	0.00	26	9	0	0	3,607	18	2280.59
218+50	21850.00	5.00	134.46	48.33	0.81	0.00	26	9	0	0	3,633	18	2297.14
218+55	21855.00	5.00	129.56	48.33	1.44	0.00	24	9	0	0	3,657	18	2312.36
218+60	21860.00	5.00	134.53	48.33	0.67	0.00	24	9	0	0	3,682	18	2327.60
218+65	21865.00	5.00	132.50	48.33	0.52	0.00	25	9	0	0	3,706	19	2343.23
218+69.69	21869.69	4.69	129.67	48.33	0.51	0.00	23	8	0	0	3,729	19	2357.50
					Column Totals		3,729	1,353	14	0			

S MAPLE STREET EARTHWORK

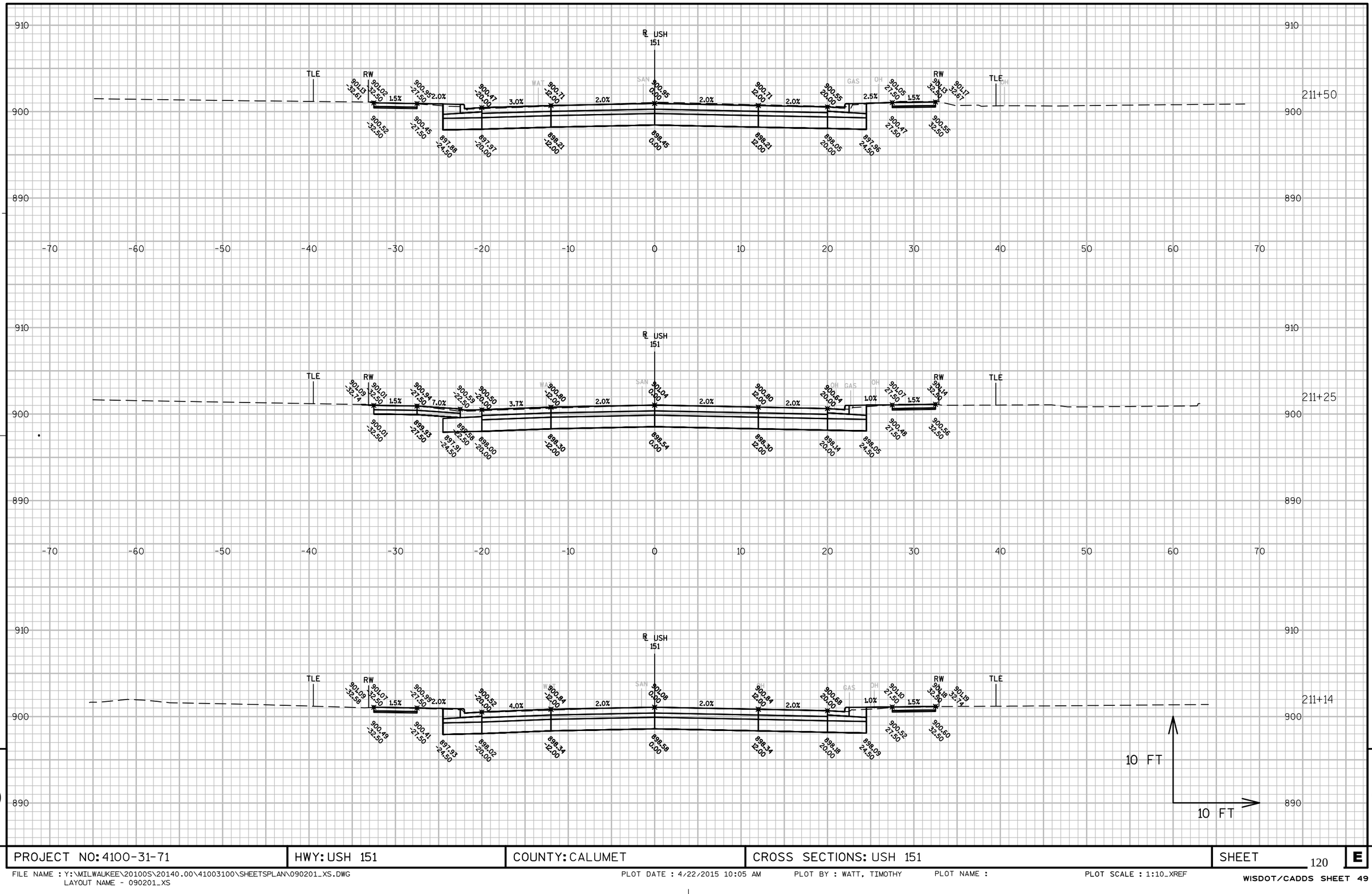
STATION	Real Station	Distance	AREA (SF) Cut	AREA (SF) Unusable Pavement Material	AREA (SF) Fill	AREA (SF) EBS	Incremental Vol (CY) (Unadjusted) Cut	Incremental Vol (CY) (Unadjusted) Unusable Pavement Material	Incremental Vol (CY) (Unadjusted) Fill	Incremental Vol (CY) (Unadjusted) EBS	Cumulative Vol (CY) Cut 1.00	Cumulative Vol (CY) Expanded Fill 1.33	Mass Ordinate
15+20.01	1520.01	0.00	0.14	37.00	0.00	0.00	0	0	0	0	0	0	0.00
15+25.00	1525.00	4.99	45.75	37.00	0.00	0.00	4	7	0	0	4	0	-2.60
15+30.00	1530.00	5.00	96.77	37.00	0.00	0.00	13	7	0	0	17	0	3.75
15+35.00	1535.00	5.00	103.45	37.00	0.01	0.00	19	7	0	0	36	0	15.44
15+40.00	1540.00	5.00	109.48	37.00	0.09	0.00	20	7	0	0	56	0	28.29
15+42.30	1542.30	2.30	106.18	37.00	0.11	0.00	9	3	0	0	65	0	34.30
15+45.00	1545.00	2.70	104.04	37.00	0.11	0.00	11	4	0	0	75	0	41.10
15+47.91	1547.91	2.91	103.90	37.00	0.10	0.00	11	4	0	0	87	0	48.32
				Column Totals			87	38	0	0			

N MAPLE STREET EARTHWORK

STATION	Real Station	Distance	AREA (SF) Cut	AREA (SF) Unusable Pavement Material	AREA (SF) Fill	AREA (SF) EBS	Incremental Vol (CY) (Unadjusted) Cut	Incremental Vol (CY) (Unadjusted) Unusable Pavement Material	Incremental Vol (CY) (Unadjusted) Fill	Incremental Vol (CY) (Unadjusted) EBS	Cumulative Vol (CY) Cut 1.00	Cumulative Vol (CY) Expanded Fill 1.33	Mass Ordinate
10+24.03	1024.03	0.00	101.20	36.67	0.00	0.00	0	0	0	0	0	0	0.00
10+25.00	1025.00	0.97	101.80	36.67	0.00	0.00	4	1	0	0	4	0	2.32
10+30.00	1030.00	5.00	98.22	36.67	0.01	0.00	19	7	0	0	22	0	14.05
10+34.99	1034.99	4.99	103.24	36.67	0.01	0.00	19	7	0	0	41	0	25.89
10+35.00	1035.00	0.01	103.25	36.67	0.01	0.00	0	0	0	0	41	0	25.91
10+40.00	1040.00	5.00	105.80	36.67	0.03	0.00	19	7	0	0	60	0	38.47
10+45.00	1045.00	5.00	72.84	36.67	0.00	0.00	17	7	0	0	77	0	48.22
10+49.25	1049.25	4.25	0.26	36.67	0.00	0.00	6	6	0	0	82	0	48.20
				Column Totals			82	34	0	0			

ELM STREET EARTHWORK

STATION	Real Station	Distance	AREA (SF) Cut	AREA (SF) Salvaged / Unusable Pavement Material	AREA (SF) Fill	AREA (SF) EBS	Incremental Vol (CY) (Unadjusted) Cut	Incremental Vol (CY) (Unadjusted) Salvaged / Unusable Pavement Material	Incremental Vol (CY) (Unadjusted) Fill	Incremental Vol (CY) (Unadjusted) EBS	Cumulative Vol (CY) Cut 1.00	Cumulative Vol (CY) Expanded Fill 1.33	Mass Ordinate
20+25.36	2025.36	0.00	91.89	38.25	1.05	0.00	0	0	0	0	0	0	0.00
20+29.34	2029.34	3.99	103.42	38.25	0.00	0.00	14	6	0	0	14	0	8.67
20+30.00	2030.00	0.66	101.60	38.25	0.00	0.00	2	1	0	0	17	0	10.23
20+35.00	2035.00	5.00	104.86	38.25	0.01	0.00	19	7	0	0	36	0	22.26
20+40.00	2040.00	5.00	91.36	38.25	0.19	0.00	18	7	0	0	54	0	33.32
20+45.00	2045.00	5.00	68.80	38.25	0.00	0.00	15	7	0	0	69	0	41.04
20+50.00	2050.00	5.00	28.09	38.25	0.00	0.00	9	7	0	0	78	0	42.93
20+53.54	2053.54	3.54	0.01	38.25	0.00	0.00	2	5	0	0	80	0	39.76
				Column Totals			80	40	0	0			

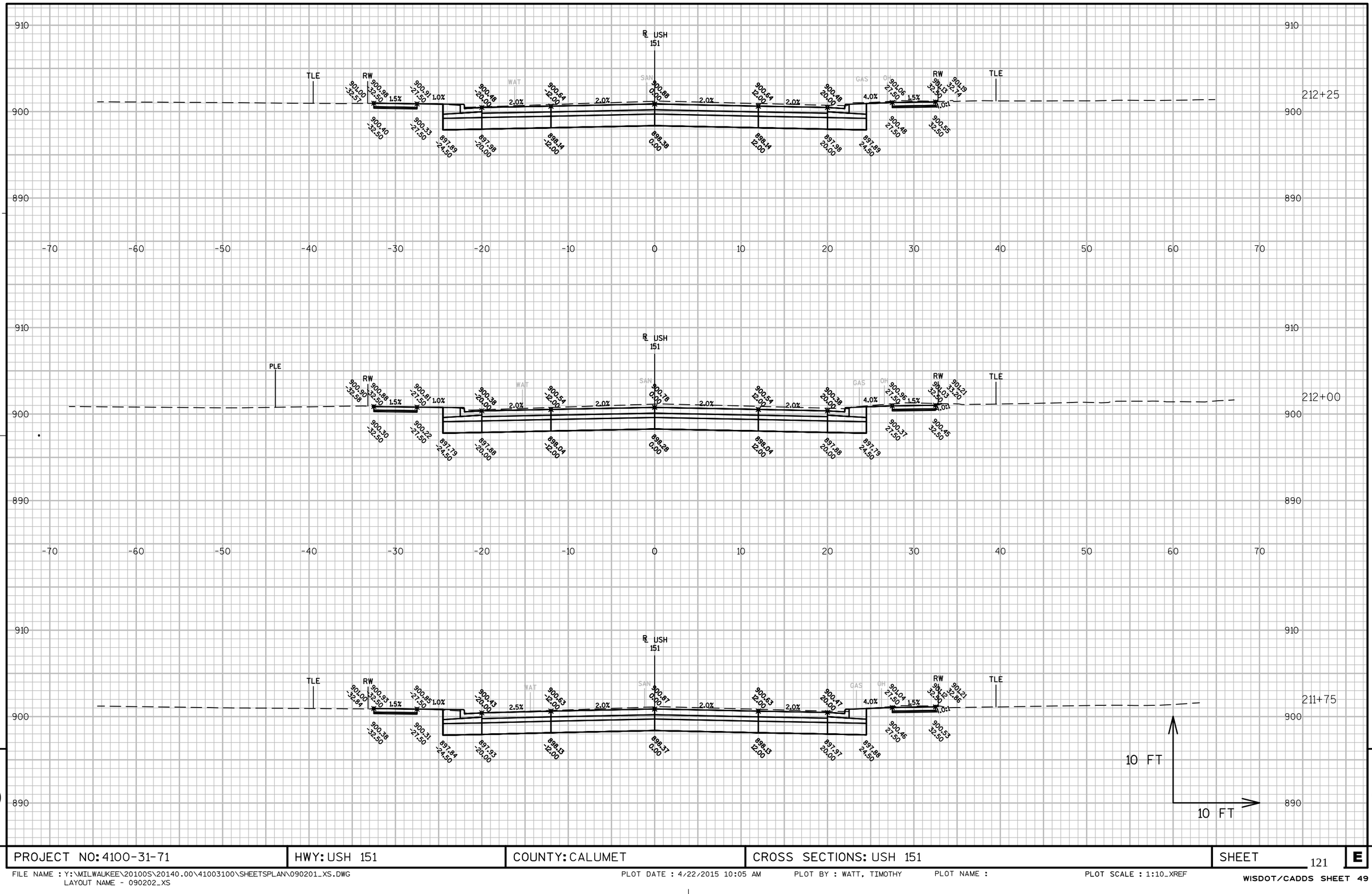


PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 120	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090201_XS

PLOT DATE : 4/22/2015 10:05 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49

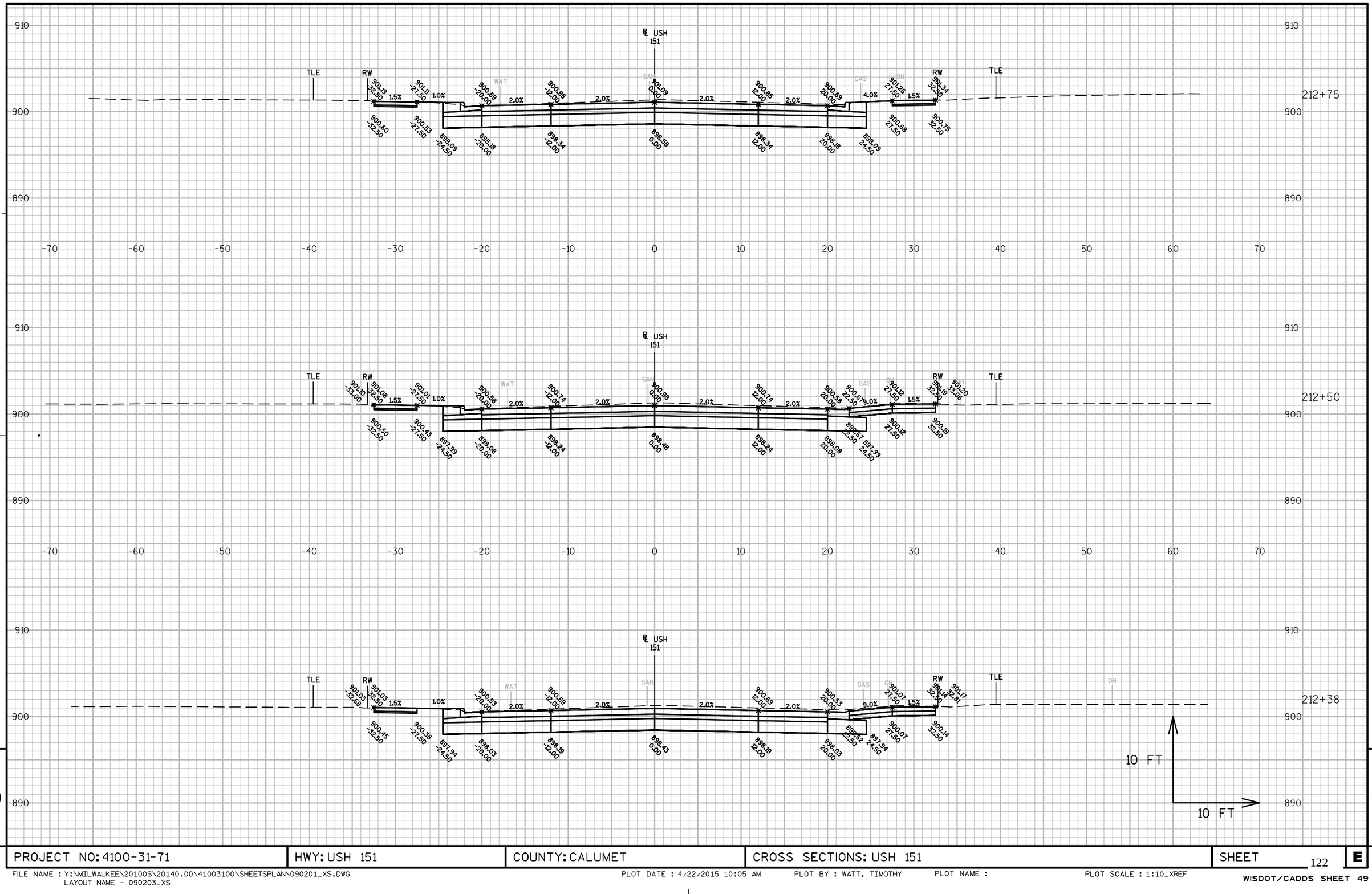


PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 121	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090202_XS

PLOT DATE : 4/22/2015 10:05 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49

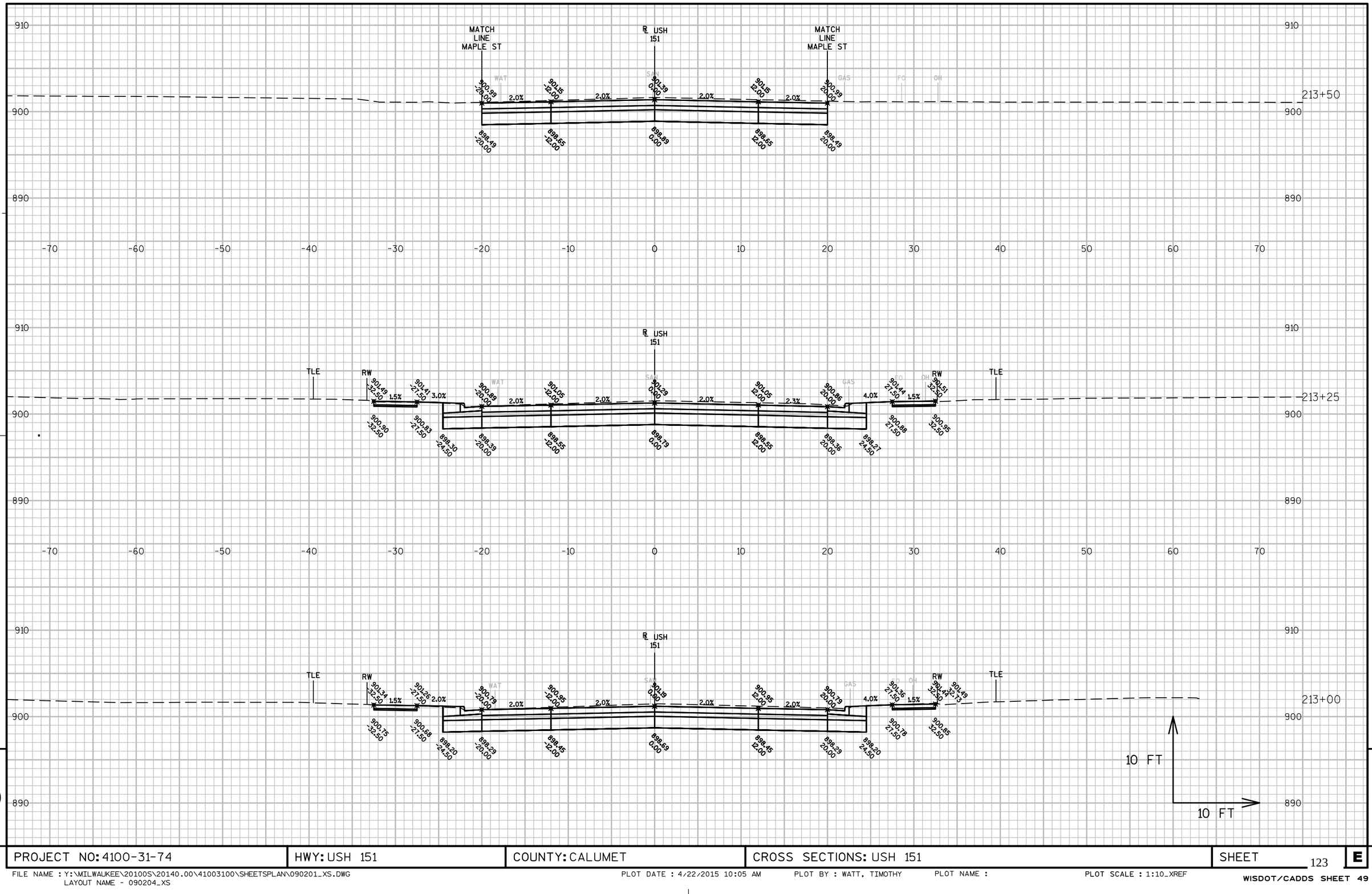


PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 122	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090203_XS

PLOT DATE : 4/22/2015 10:05 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49

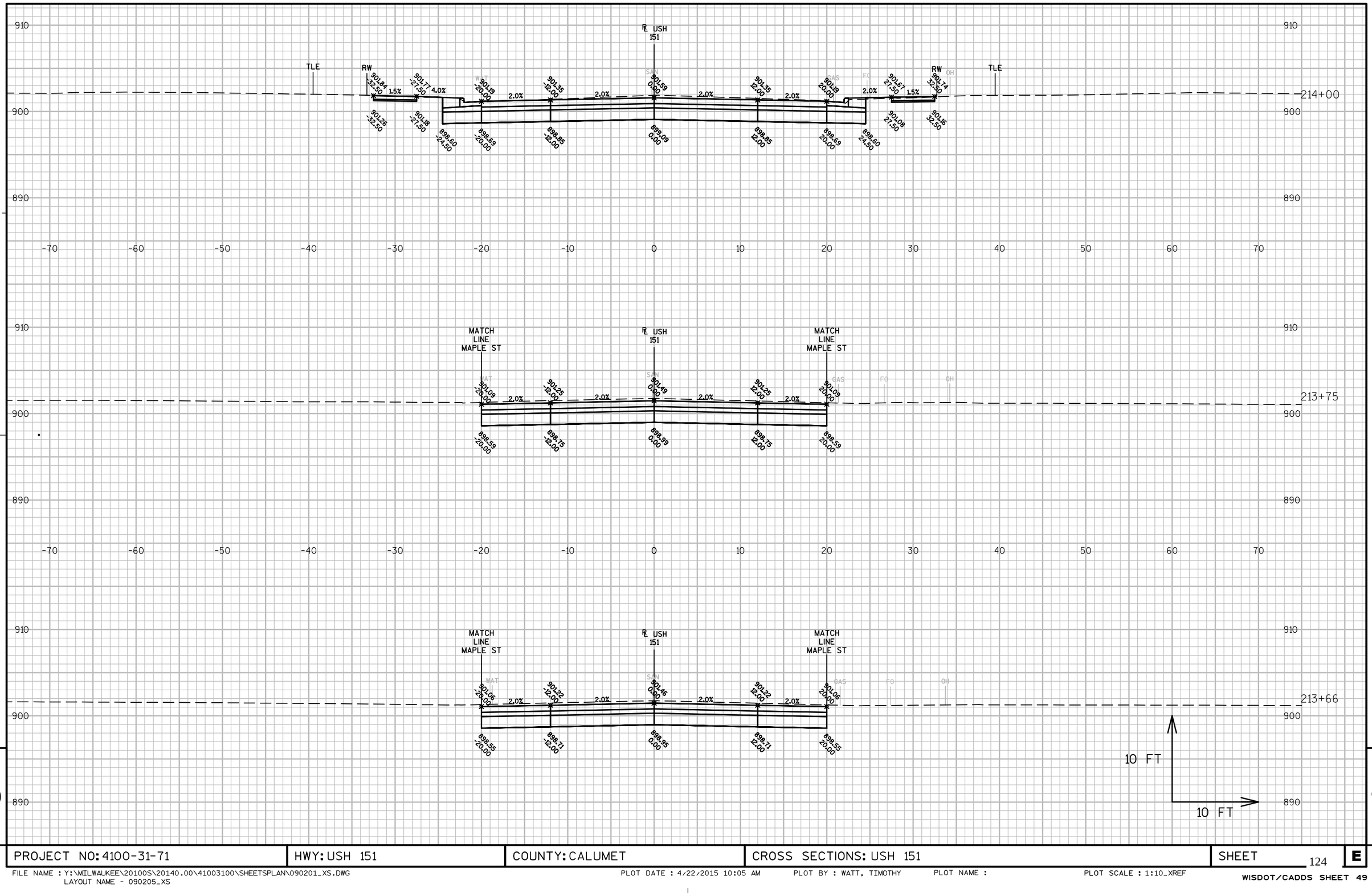


PROJECT NO: 4100-31-74	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 123	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090204_XS

PLOT DATE : 4/22/2015 10:05 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49

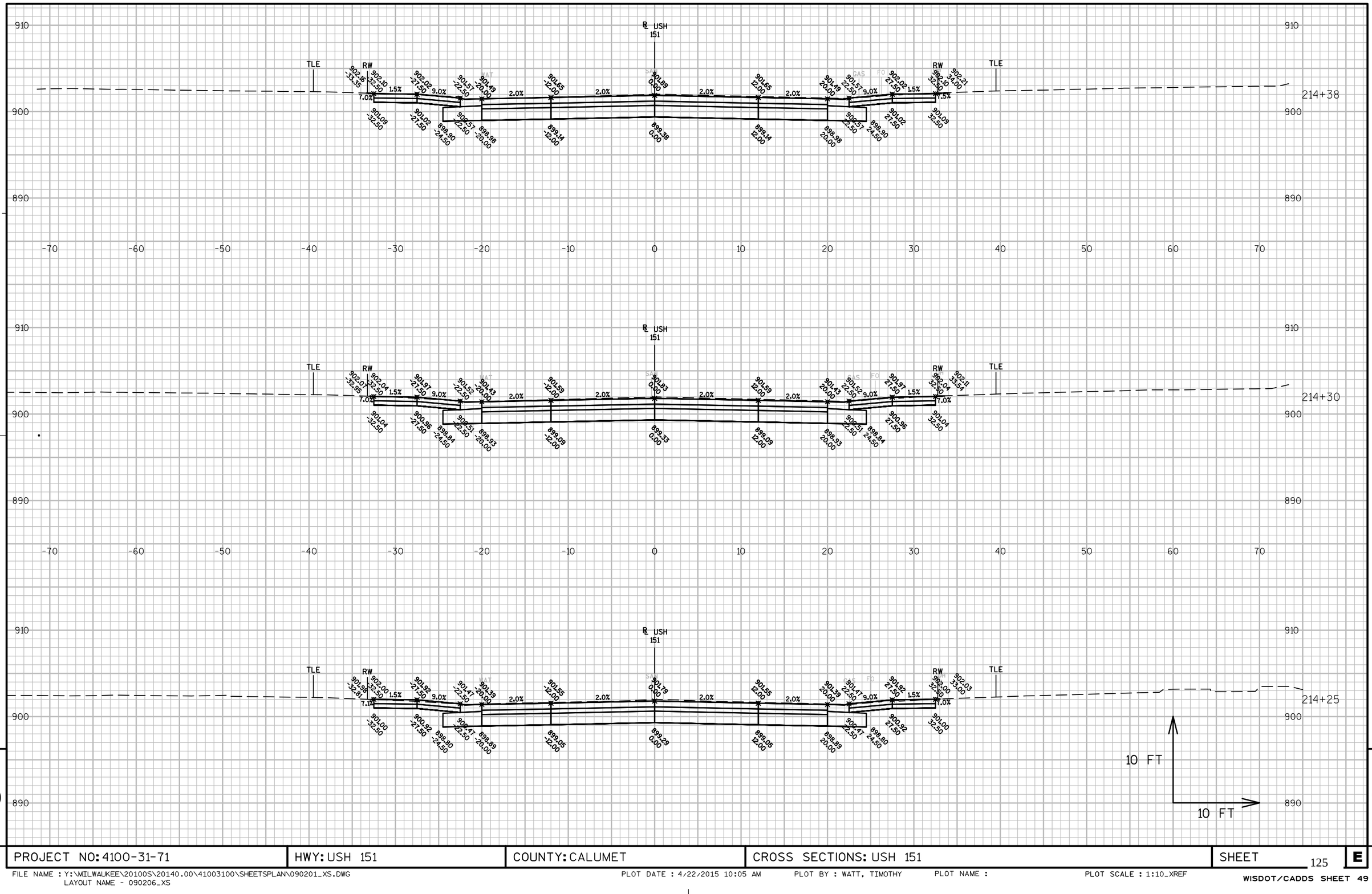


PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 124	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090205_XS

PLOT DATE : 4/22/2015 10:05 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49



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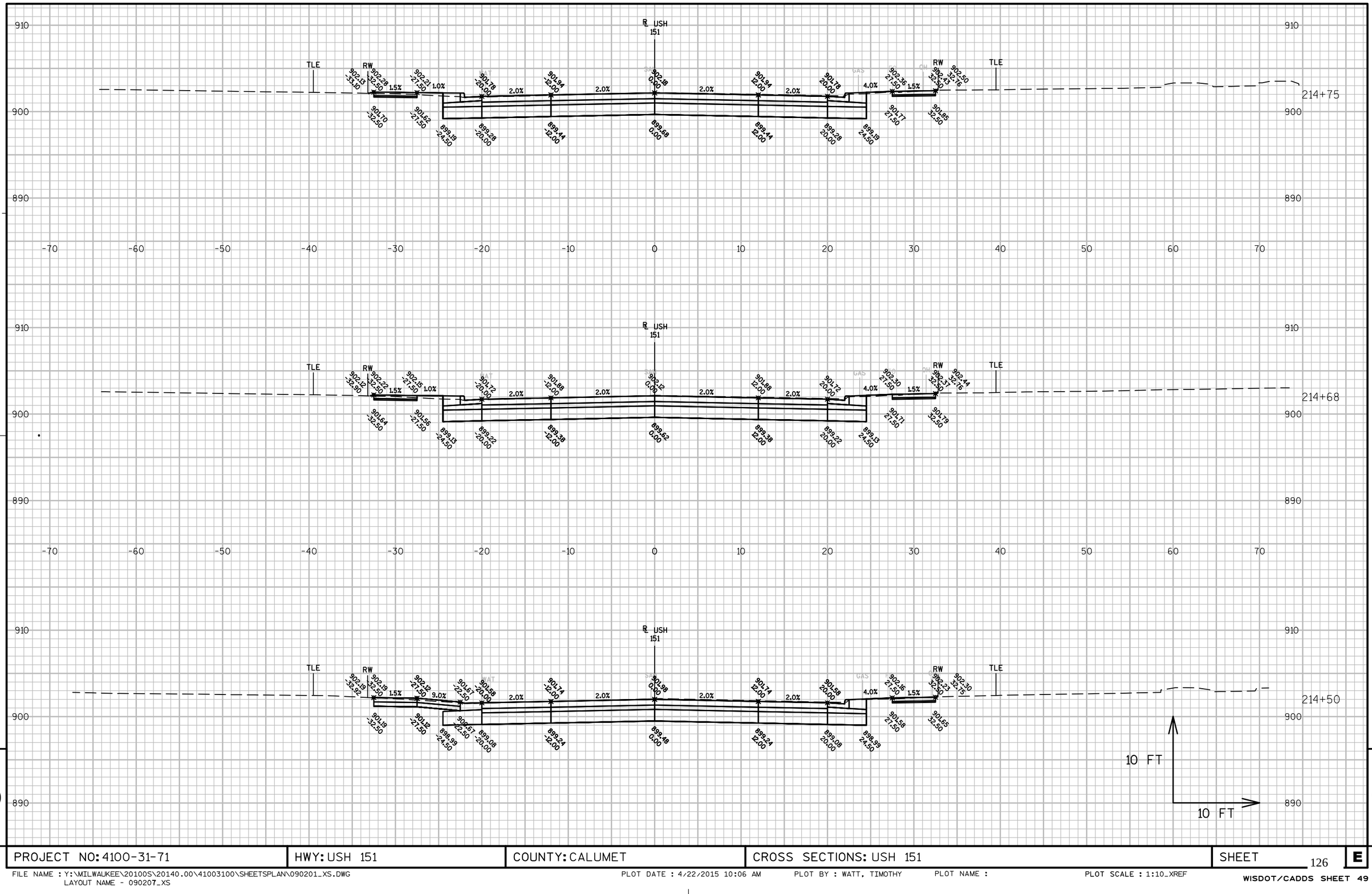
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 125	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090206_XS

PLOT DATE : 4/22/2015 10:05 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF



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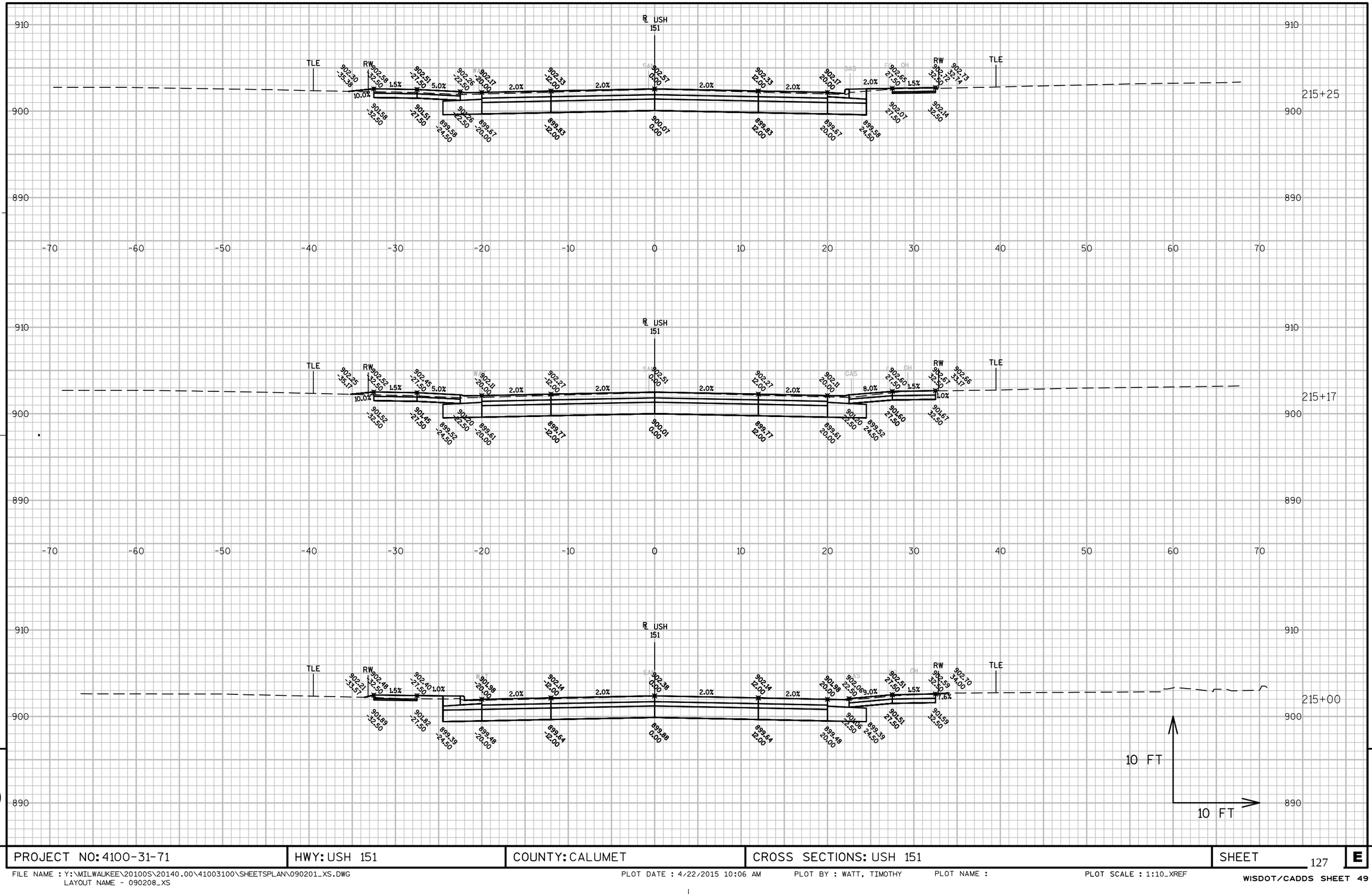
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 126	E

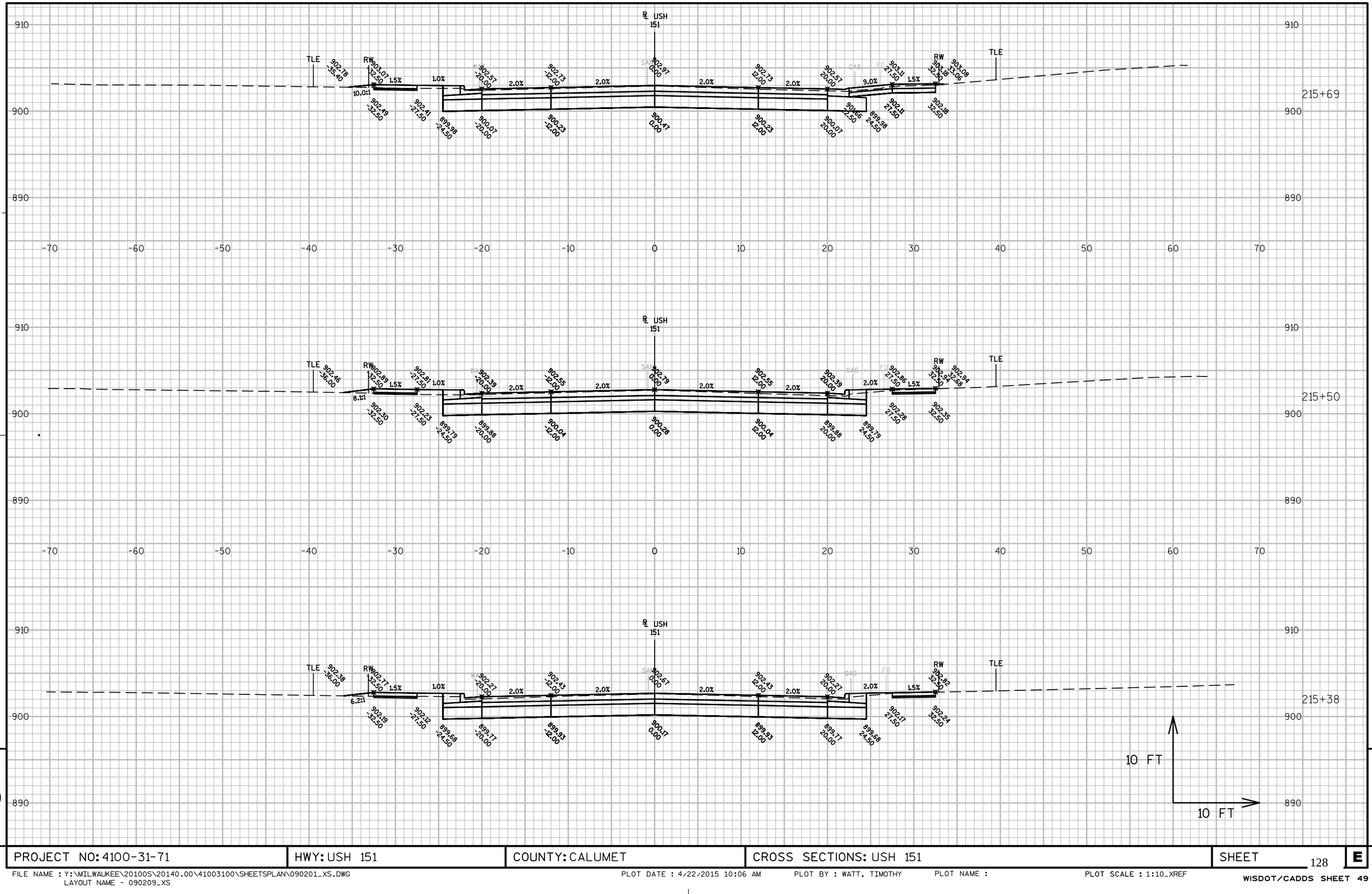


PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 127	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090208_XS

PLOT DATE : 4/22/2015 10:06 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49



PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 128	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090209_XS

PLOT DATE : 4/22/2015 10:06 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

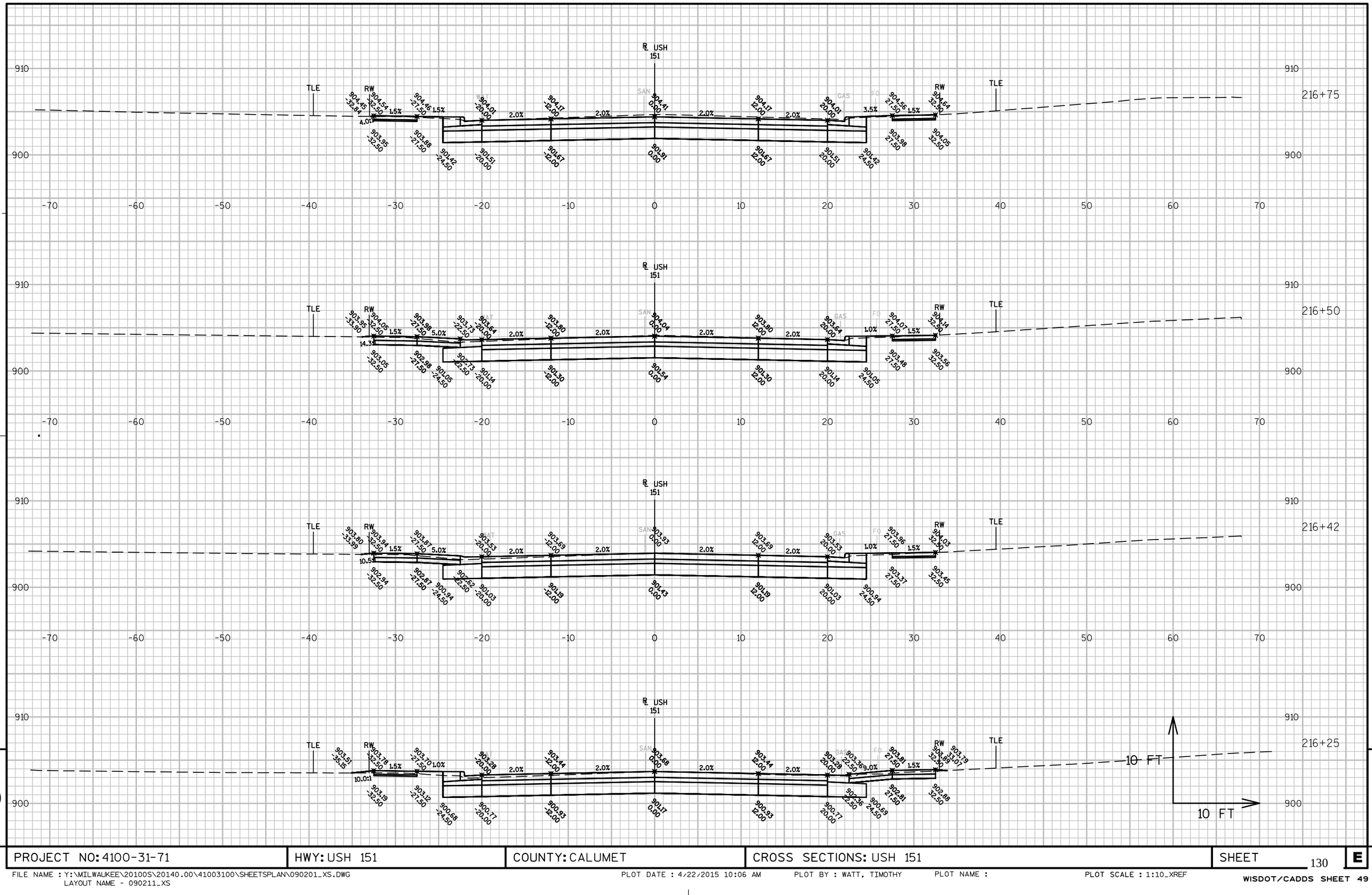
WISDOT/CADDS SHEET 49

PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 129	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090210_XS

PLOT DATE : 4/22/2015 10:06 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

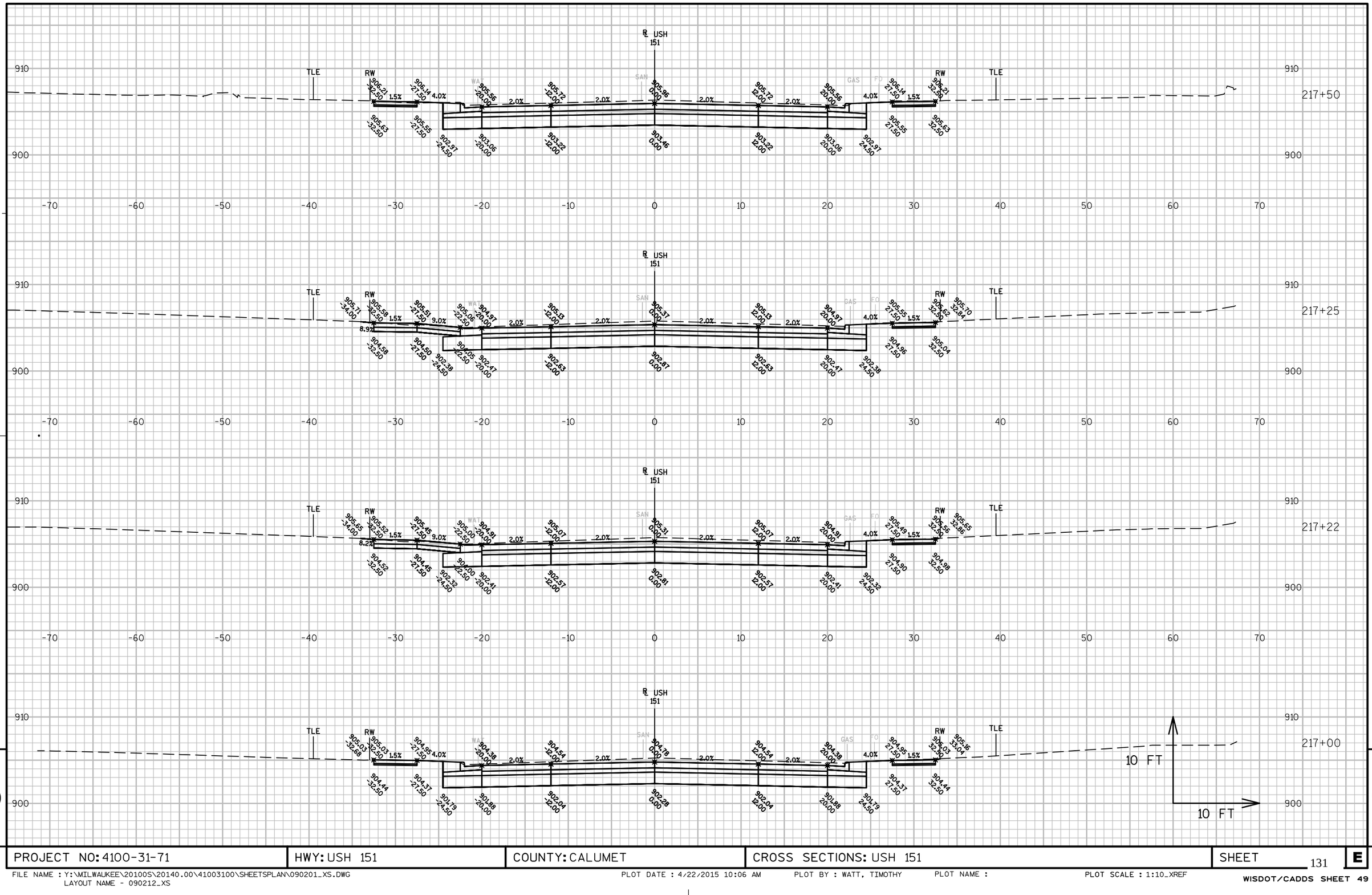
WISDOT/CADDS SHEET 49



PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 130	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090211_XS

PLOT DATE : 4/22/2015 10:06 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF



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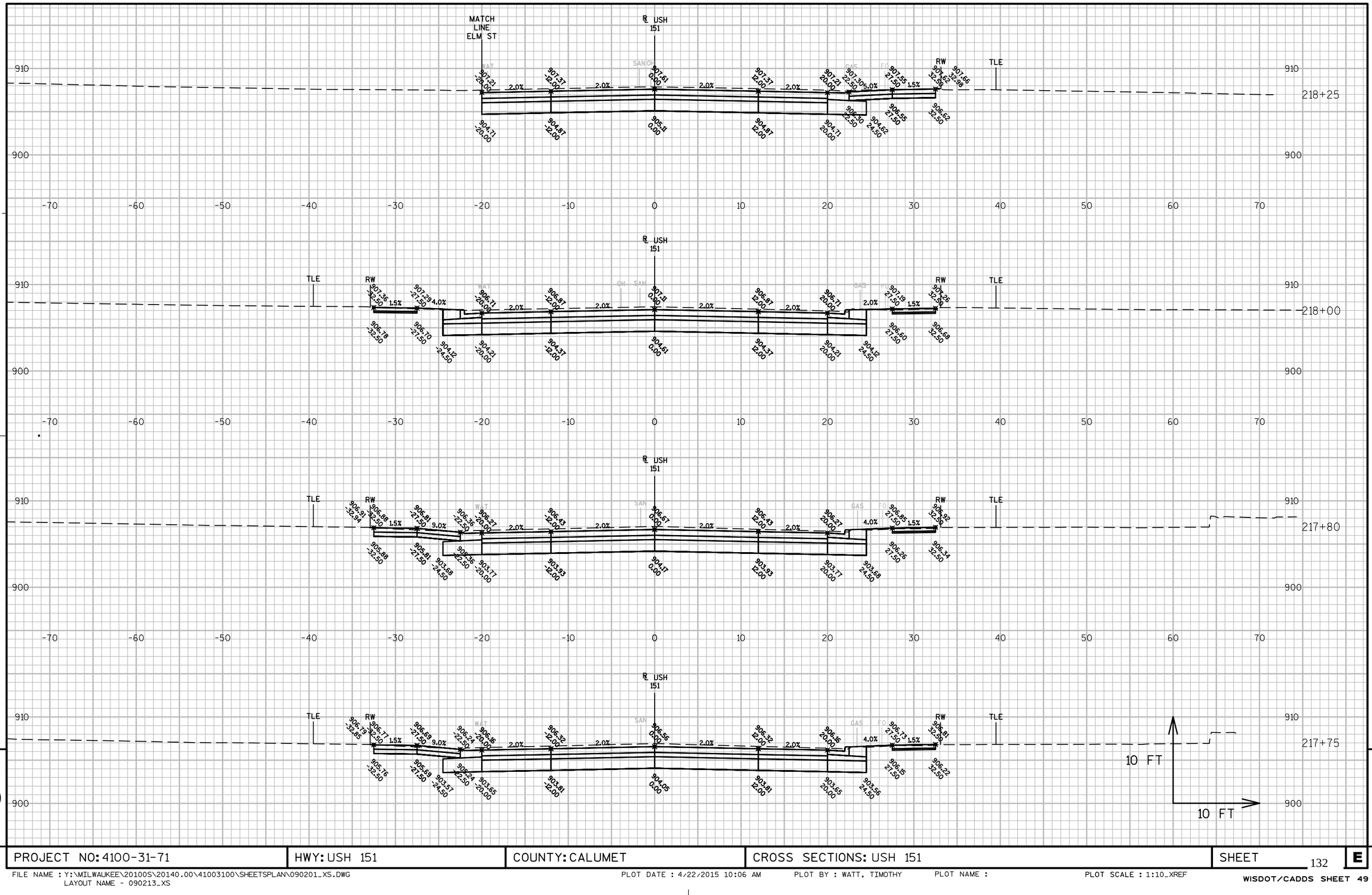
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 131	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090212_XS
PLOT DATE : 4/22/2015 10:06 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF
WISDOT/CADDS SHEET 49



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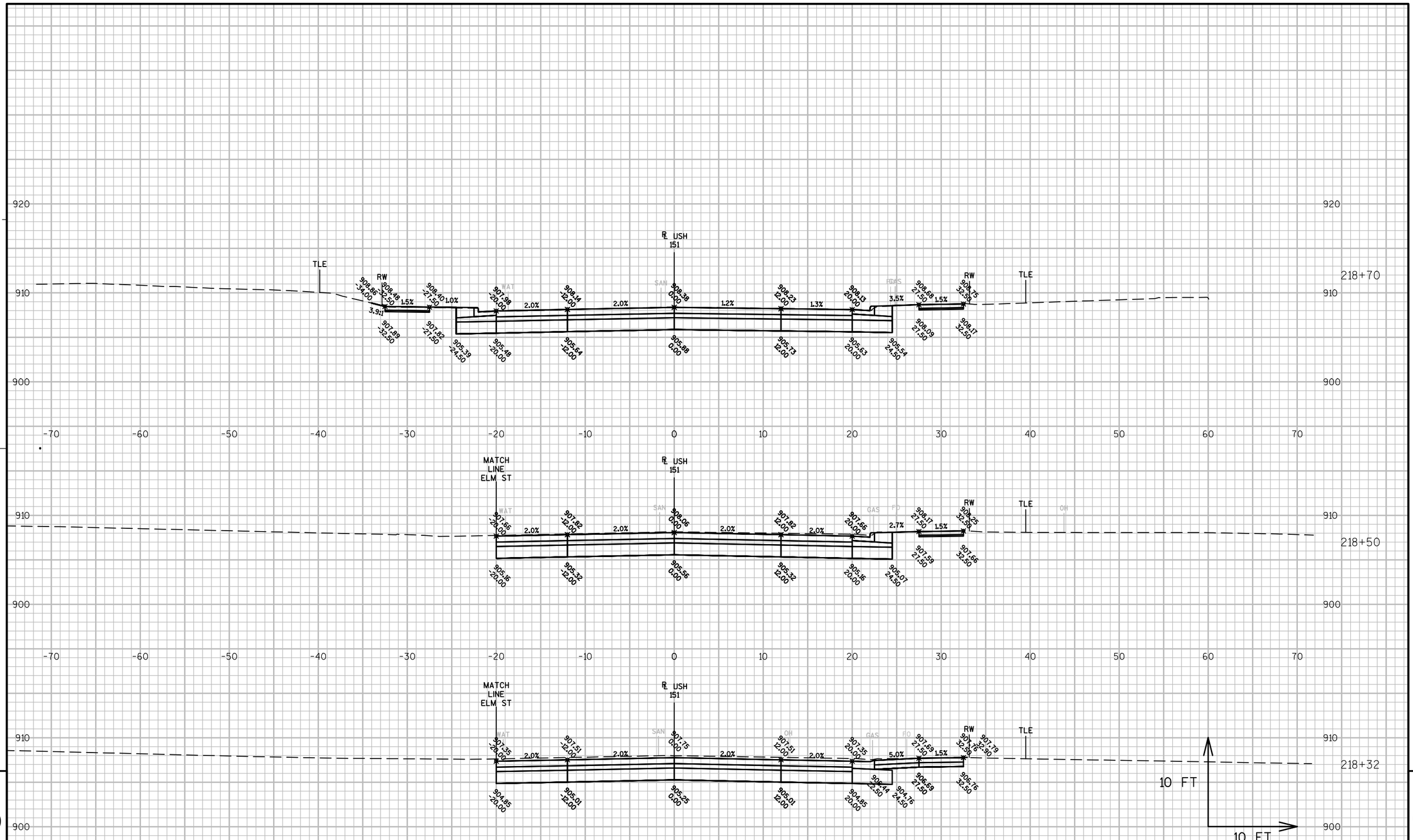
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 132	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090213_XS

PLOT DATE : 4/22/2015 10:06 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49



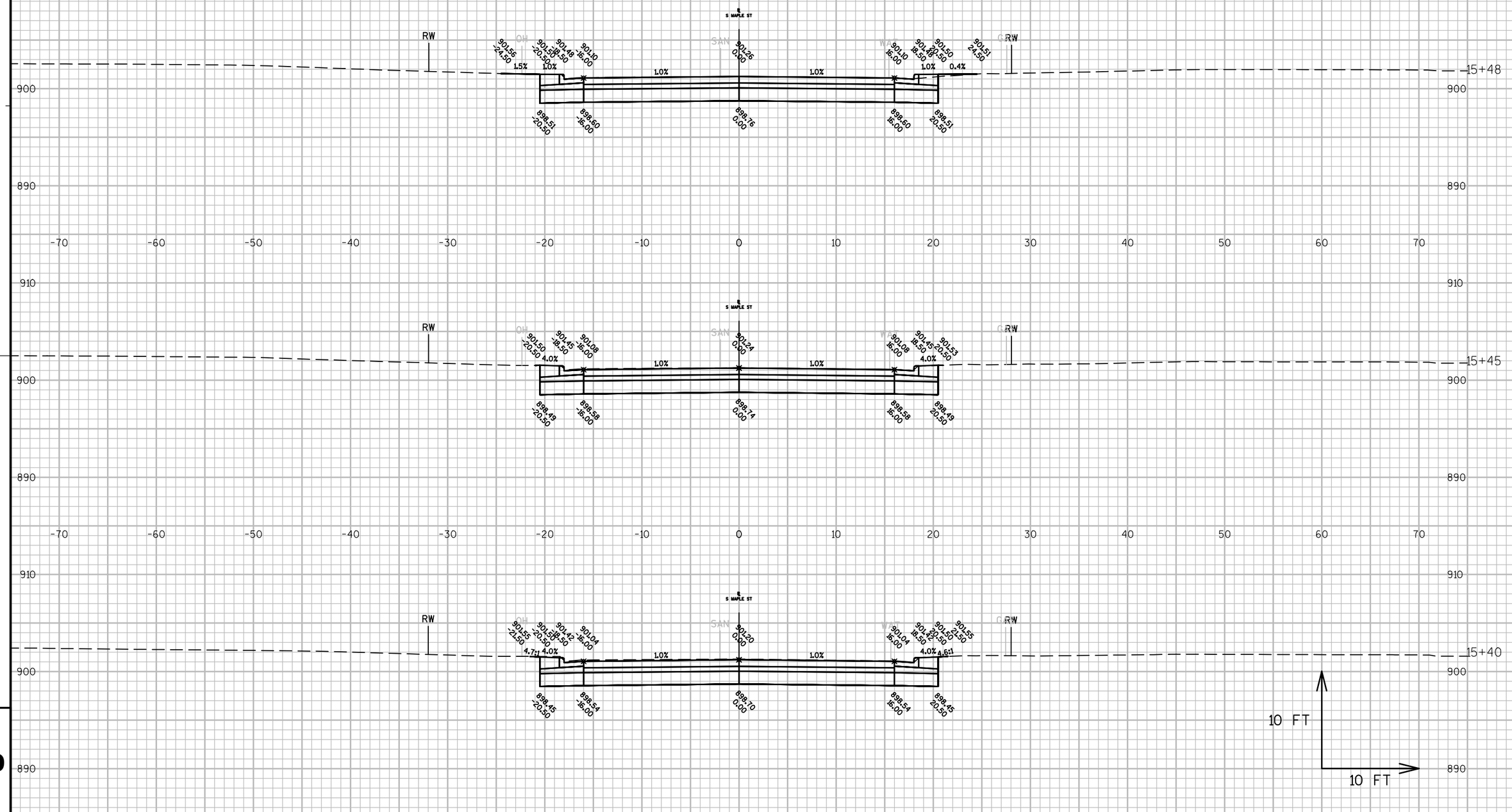
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: USH 151	SHEET 133	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 090214_XS

PLOT DATE : 4/22/2015 10:06 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49

NOTE: ℄ S MAPLE STREET ALIGNMENT HAS STATIONING THAT RUNS NORTH TO SOUTH. SEE ALIGNMENT PLAN FOR DETAILS.



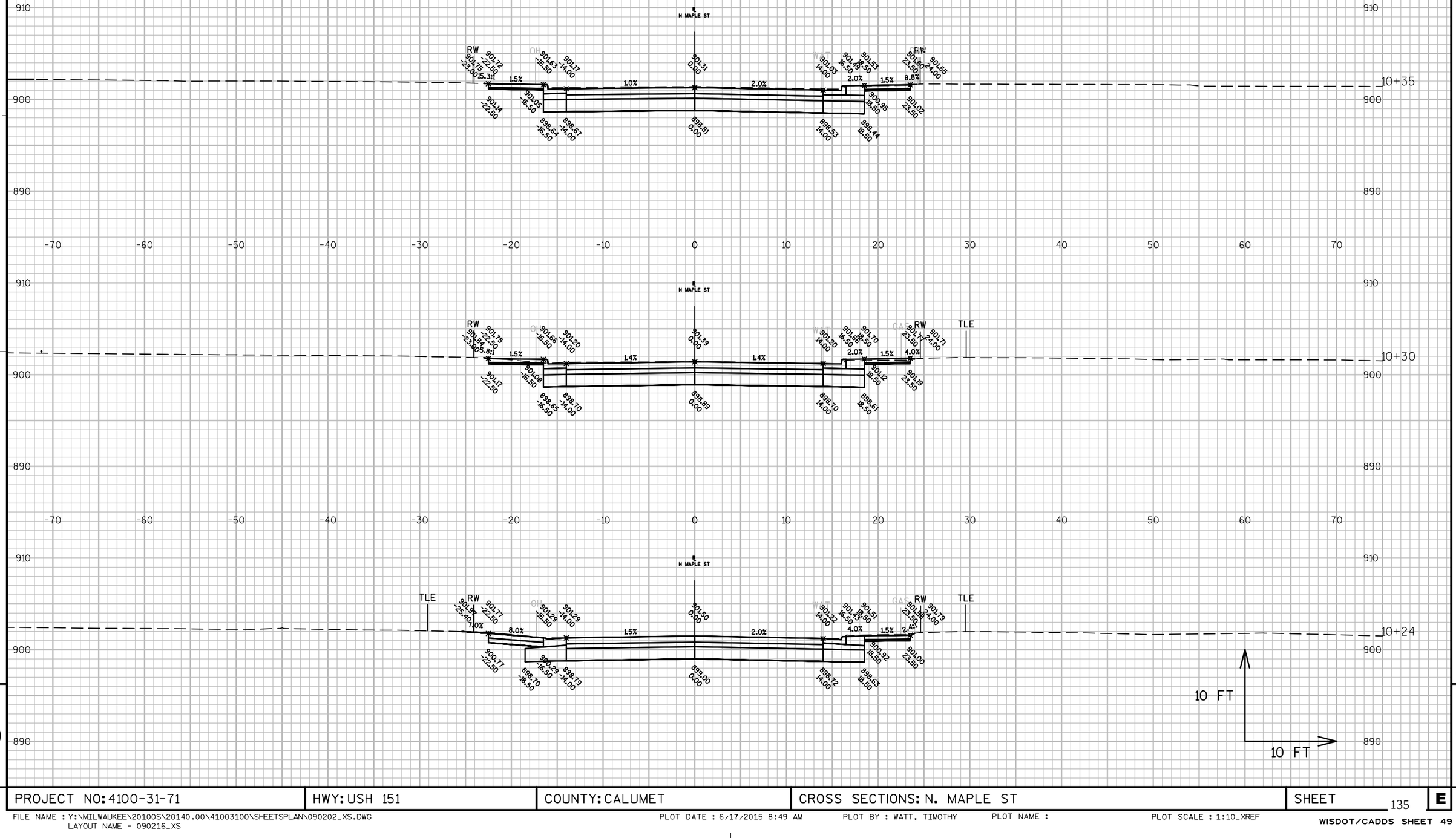
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: S. MAPLE ST	SHEET 134	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090202_XS.DWG
LAYOUT NAME - 090215_XS

PLOT DATE : 6/17/2015 8:49 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49

NOTE: ℄ N MAPLE STREET ALIGNMENT HAS STATIONING THAT RUNS NORTH TO SOUTH. SEE ALIGNMENT PLAN FOR DETAILS.



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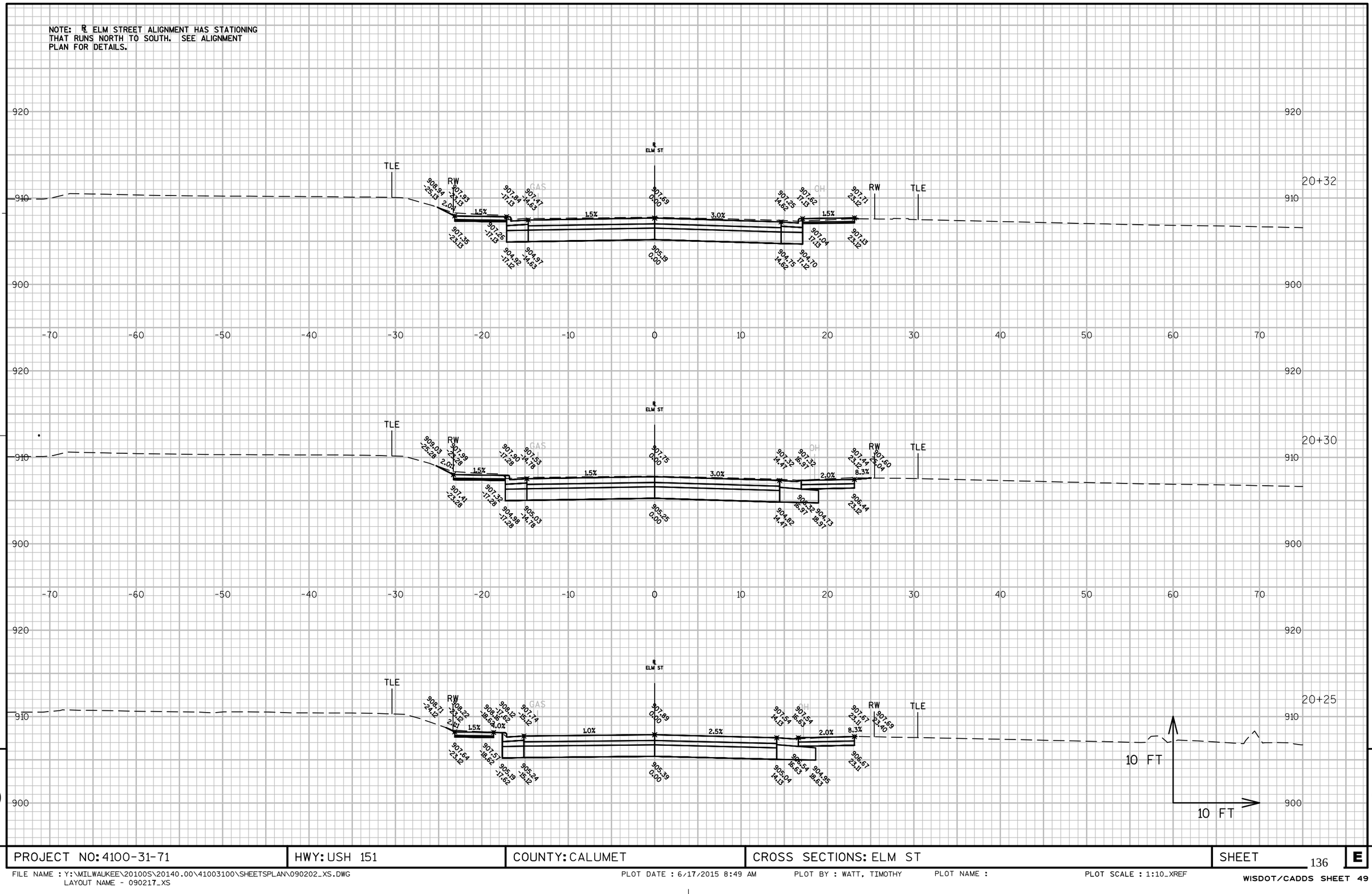
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: N. MAPLE ST	SHEET 135	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090202_XS.DWG
LAYOUT NAME - 090216_XS

PLOT DATE : 6/17/2015 8:49 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

NOTE: ℄ ELM STREET ALIGNMENT HAS STATIONING THAT RUNS NORTH TO SOUTH. SEE ALIGNMENT PLAN FOR DETAILS.



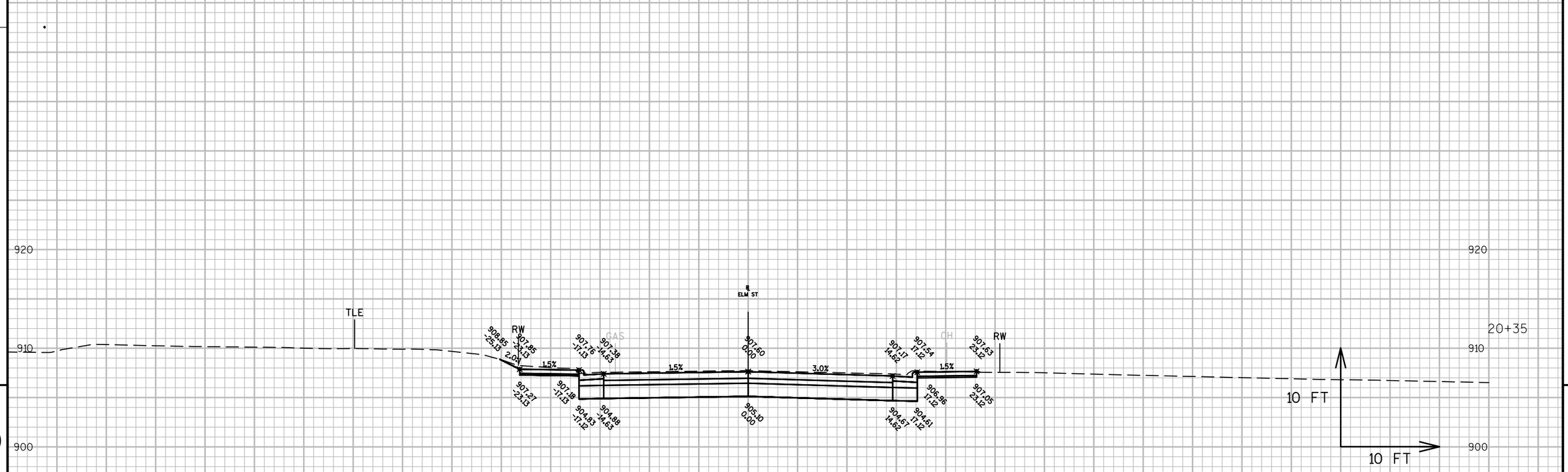
PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: ELM ST	SHEET 136	E

FILE NAME : Y:\MILWAUKEE\20100S\20140.00\41003100\SHEETSPLAN\090202_XS.DWG
LAYOUT NAME - 090217_XS

PLOT DATE : 6/17/2015 8:49 AM PLOT BY : WATT, TIMOTHY PLOT NAME : PLOT SCALE : 1:10_XREF

WISDOT/CADDS SHEET 49

NOTE: ℄ ELM STREET ALIGNMENT HAS STATIONING THAT RUNS NORTH TO SOUTH. SEE ALIGNMENT PLAN FOR DETAILS.



PROJECT NO: 4100-31-71	HWY: USH 151	COUNTY: CALUMET	CROSS SECTIONS: ELM ST	SHEET 137	E

Wisconsin Department of Transportation

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