

WKE
PROJECT ID: 4890-00-71
WITH:
COUNTY: SOUTHEAST REGION

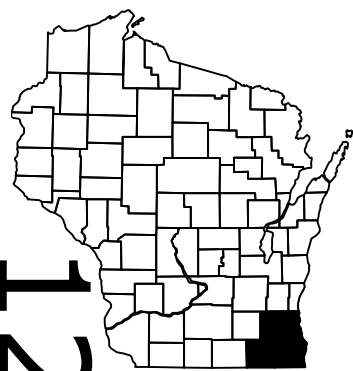
JULY 2015

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 52

BEGIN CONSTRUCTION
STH 59 (OAKRIDGE DR.)



DESIGN DESIGNATION

A.A.D.T.	=	BEGIN CONSTRUCTION
A.A.D.T.	=	STH 190 (STH 16)
D.H.V.	=	
D.D.	=	
T.	=	
DESIGN SPEED	=	
ESALS	=	

CONVENTIONAL SYMBOLS

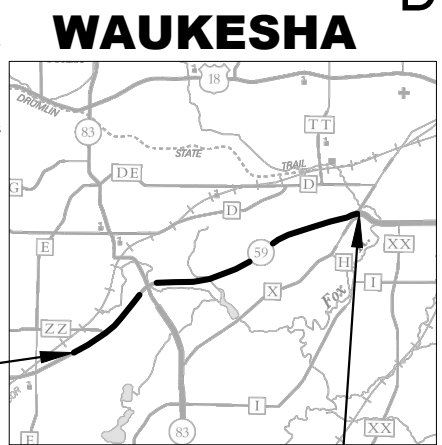
PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
	STORM SEWER
	TELEPHONE
	WATER
MARSH AREA	UTILITY PEDESTAL
	POWER POLE
WOODED OR SHRUB AREA	TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

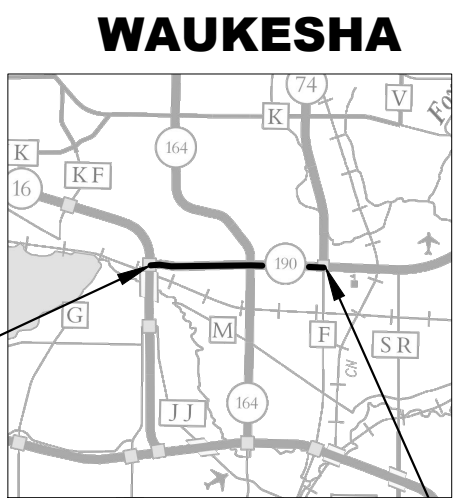
PLAN OF PROPOSED IMPROVEMENT

VARIOUS LOCATIONS
CRACK SEALING-FY 2016
VARIOUS HIGHWAYS
SOUTHEAST REGION WIDE

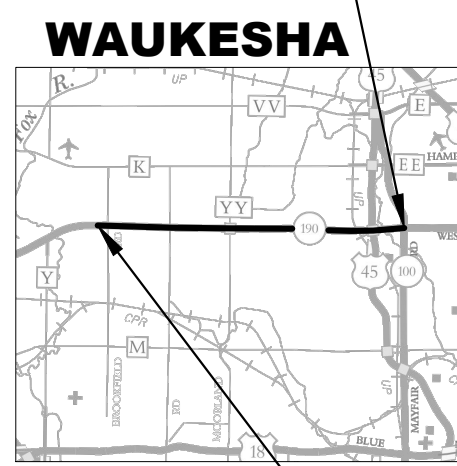
STATE PROJECT NUMBER
4890-00-71



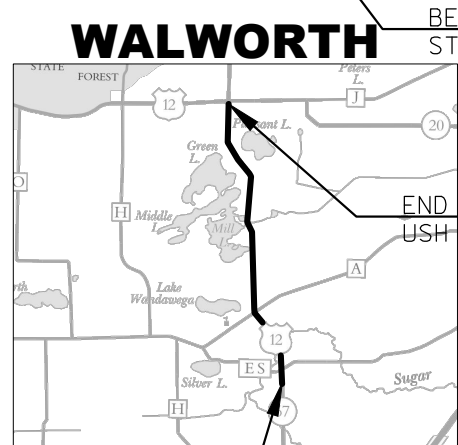
END CONSTRUCTION
STH 59 (CTH X)



END CONSTRUCTION
STH 190 (FIVE FIELDS DR.)



END CONSTRUCTION
STH 190 (STH 100)

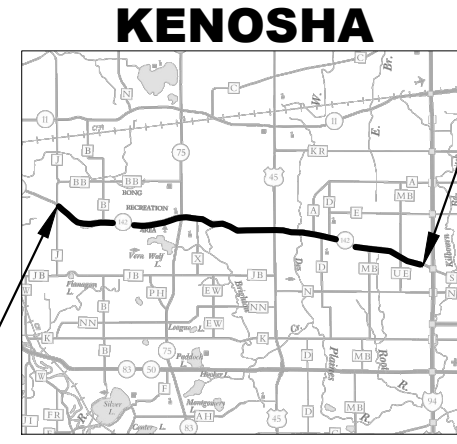


BEGIN CONSTRUCTION
USH 12 (OAK CT.)

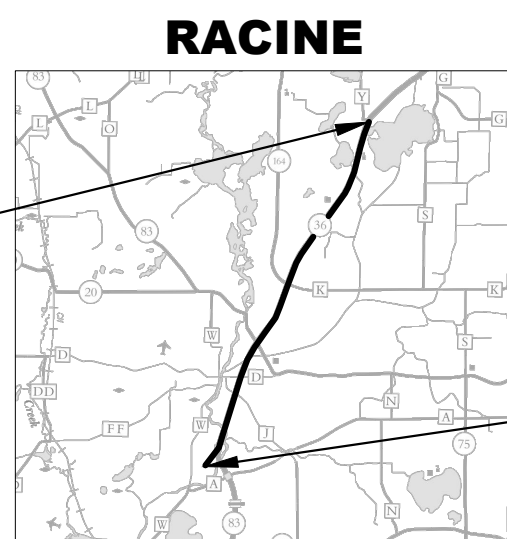
LAYOUT
SCALE 0
TOTAL NET LENGTH OF CENTERLINE =



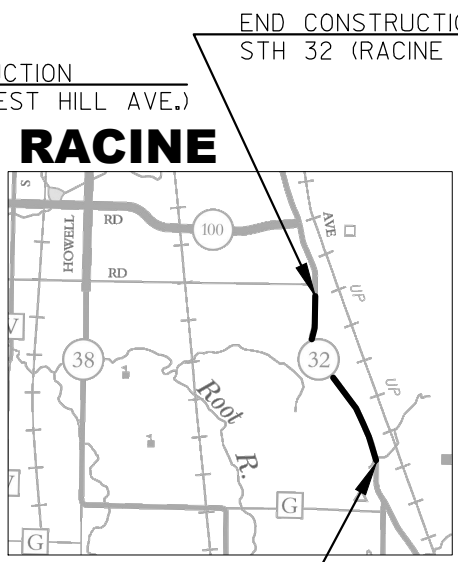
END CONSTRUCTION
STH 32 (FOREST HILL AVE.)



BEGIN CONSTRUCTION
STH 142 (CTH J)



END CONSTRUCTION
STH 36 (CTH Y)



BEGIN CONSTRUCTION
STH 36 (CTH W)

END CONSTRUCTION
STH 32 (RACINE COUNTY LINE)

BEGIN CONSTRUCTION
STH 32 (GEHRING RD.)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	SURVEYOR
Designer	PRIYA MITRA
Project Manager	JOHN KANZENBACH
Regional Examiner	RICHARD HERRICK
Regional Supervisor	BENEDICT ERUCHALU

APPROVED FOR THE DEPARTMENT

DATE: 4/17/2015

John Kanzenbach
(Signature)

E

SE REGION STATE HIGHWAY MAINTAINENCE CONTACTS:

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WALWORTH & KENOSHA
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Kristina.Betzold@wisconsin.gov
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KENOSHA,RACINE,WALWORTH ,& WAUKESHA
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WAUKESHA,WI 53188
craig.webster@wisconsin.gov
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OTHER CONTACTS
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John.kanzenbach@wi.dot.gov

GENERAL NOTES

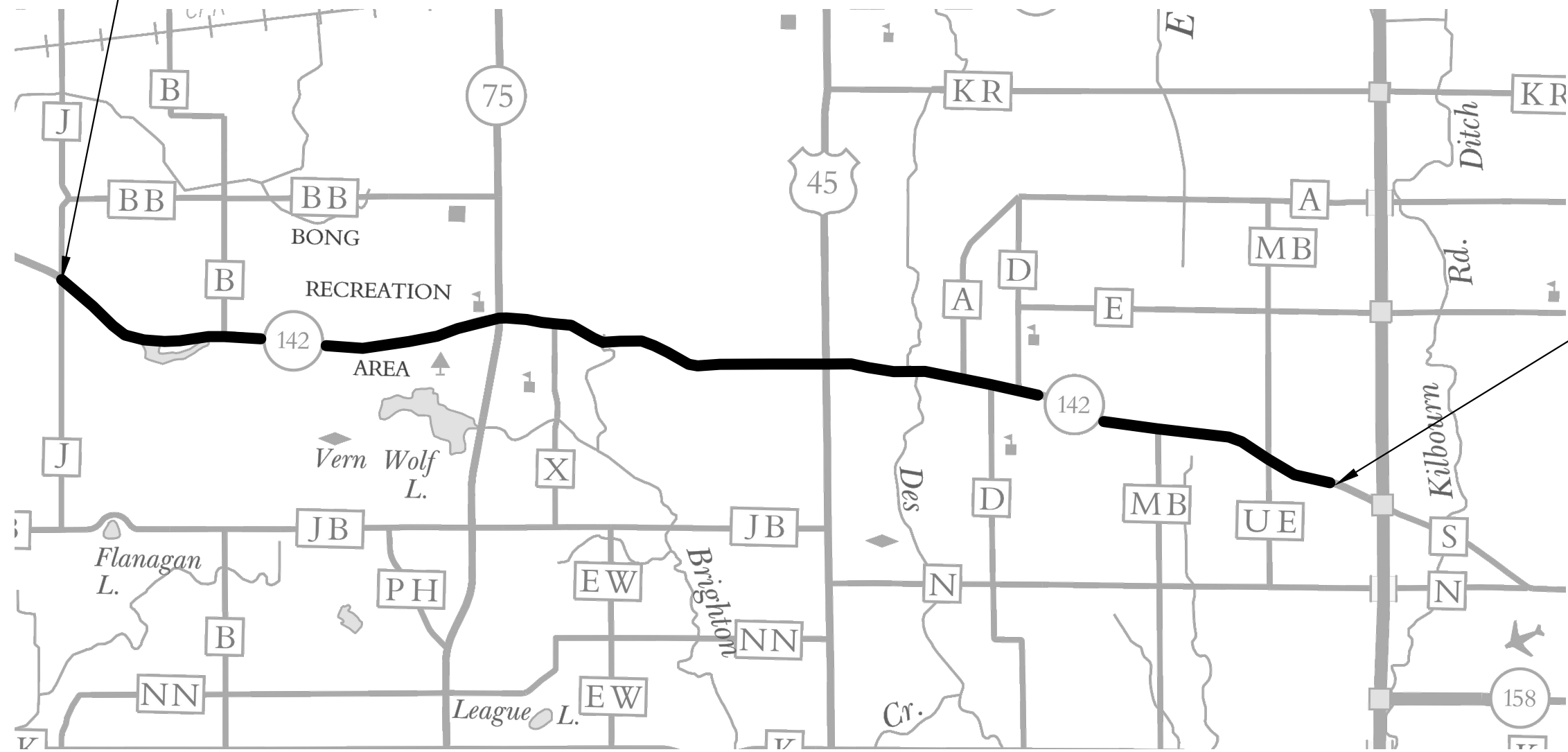
- NO UTILITY FACILITIES ARE IDENTIFIED IN THE PLAN.
- IDENTIFY ALL UTILITIES IN THE AREA BY CONTACTING DIGGERS HOTLINE.
- QUANTITY FOR CRACK SEALING IS ESTIMATED AT 4 LF PER 1 LB OF MATERIAL.
- EPOXY PAVEMENT MARKINGS WILL BE CONTINOUS WITH IN PROJECT LIMITS.
- CRACK SEALING LOCATIONS ARE APPROXIMATE AND WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- CRACK SEALING AREA INCLUDE MAINLINE TRAVEL LANES,SHOULDERS,TURNLANES AUXILLARY LANES AND TAPER SECTIONS AND RAMPS
- TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- QUANTITY FOR Rout AND SEAL IS ESTIMATED AT 3 LF PER 1 LB OF MATERIAL.



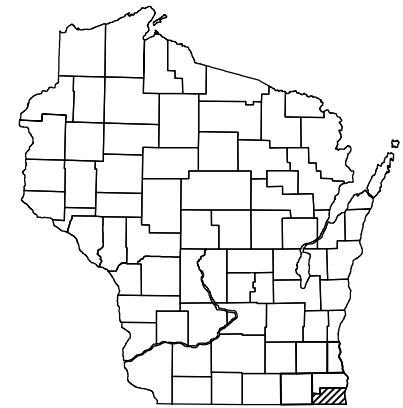
Dial **811** or (800) 242-8511
www.DiggersHotline.com

BEGIN CONSTRUCTION
STH 142 (CTH J)

KENOSHA COUNTY



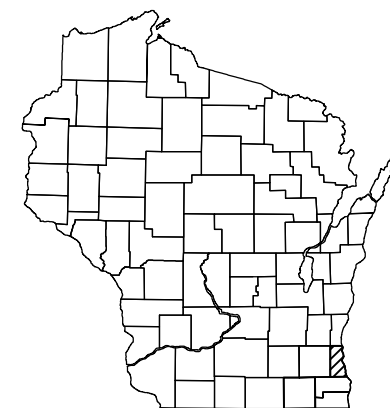
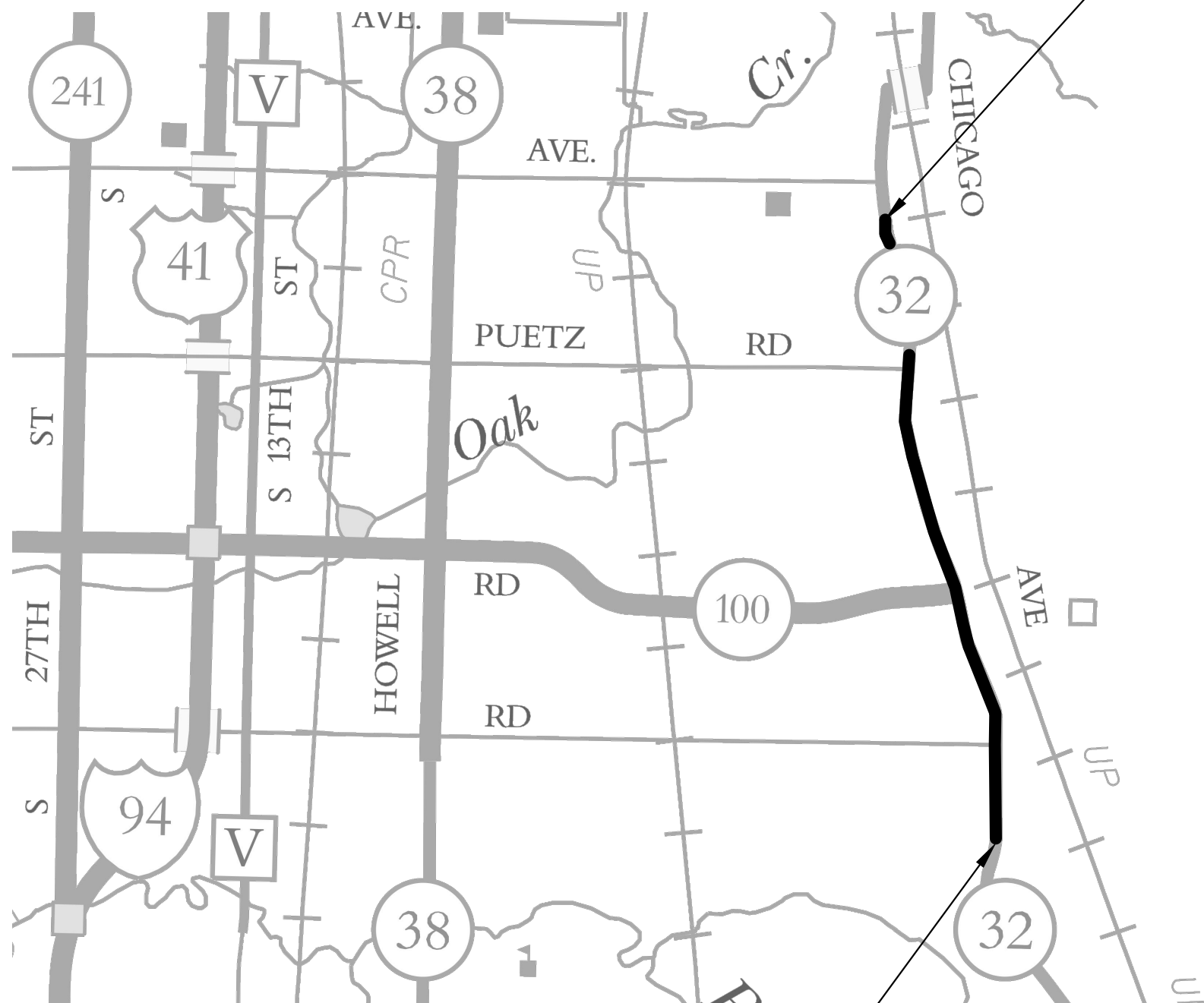
END CONSTRUCTION
STH 142 (2550' BEFORE IH-94)



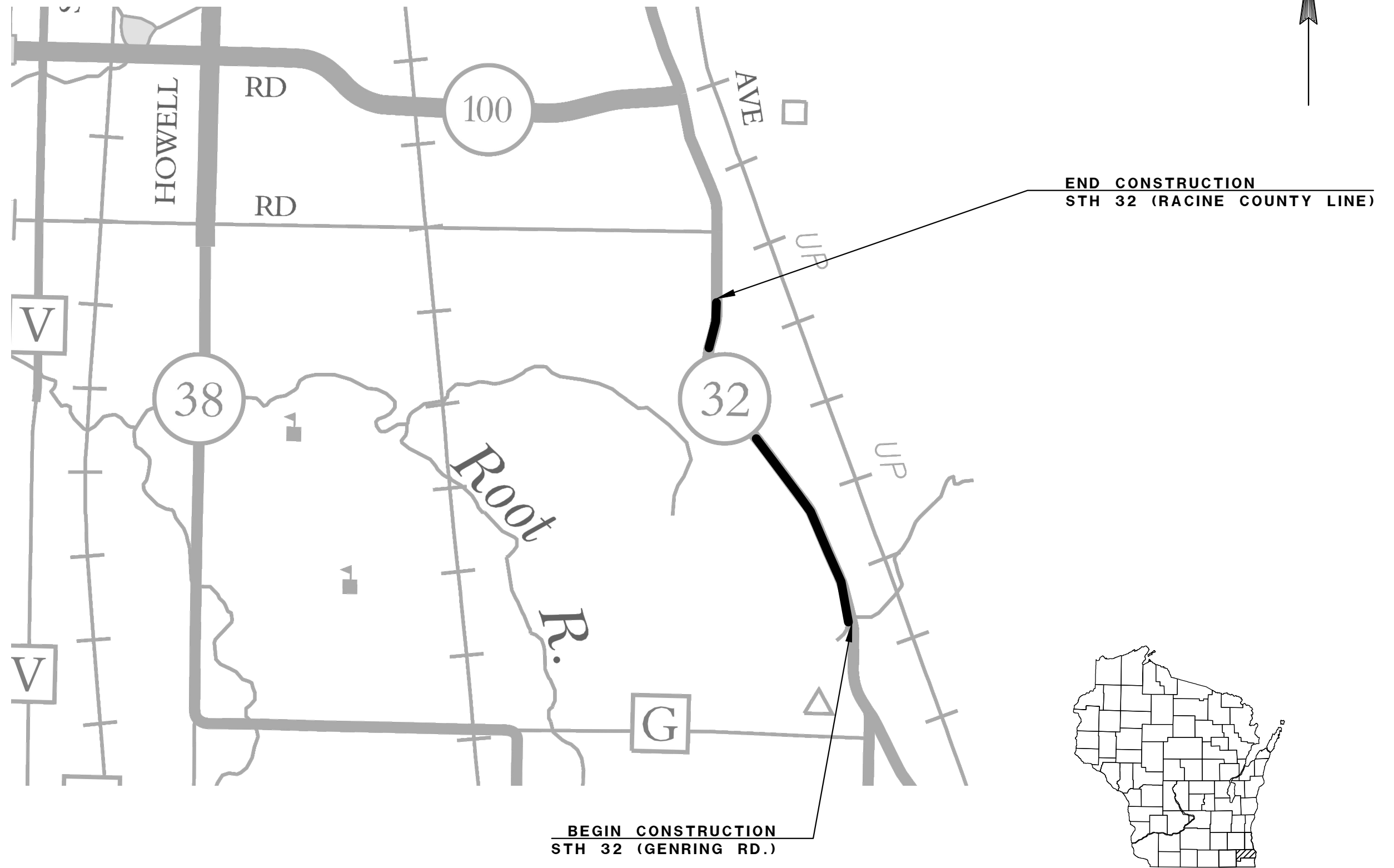
MILWAUKEE COUNTY

END CONSTRUCTION
STH 32 (FOREST HILL AVE.)

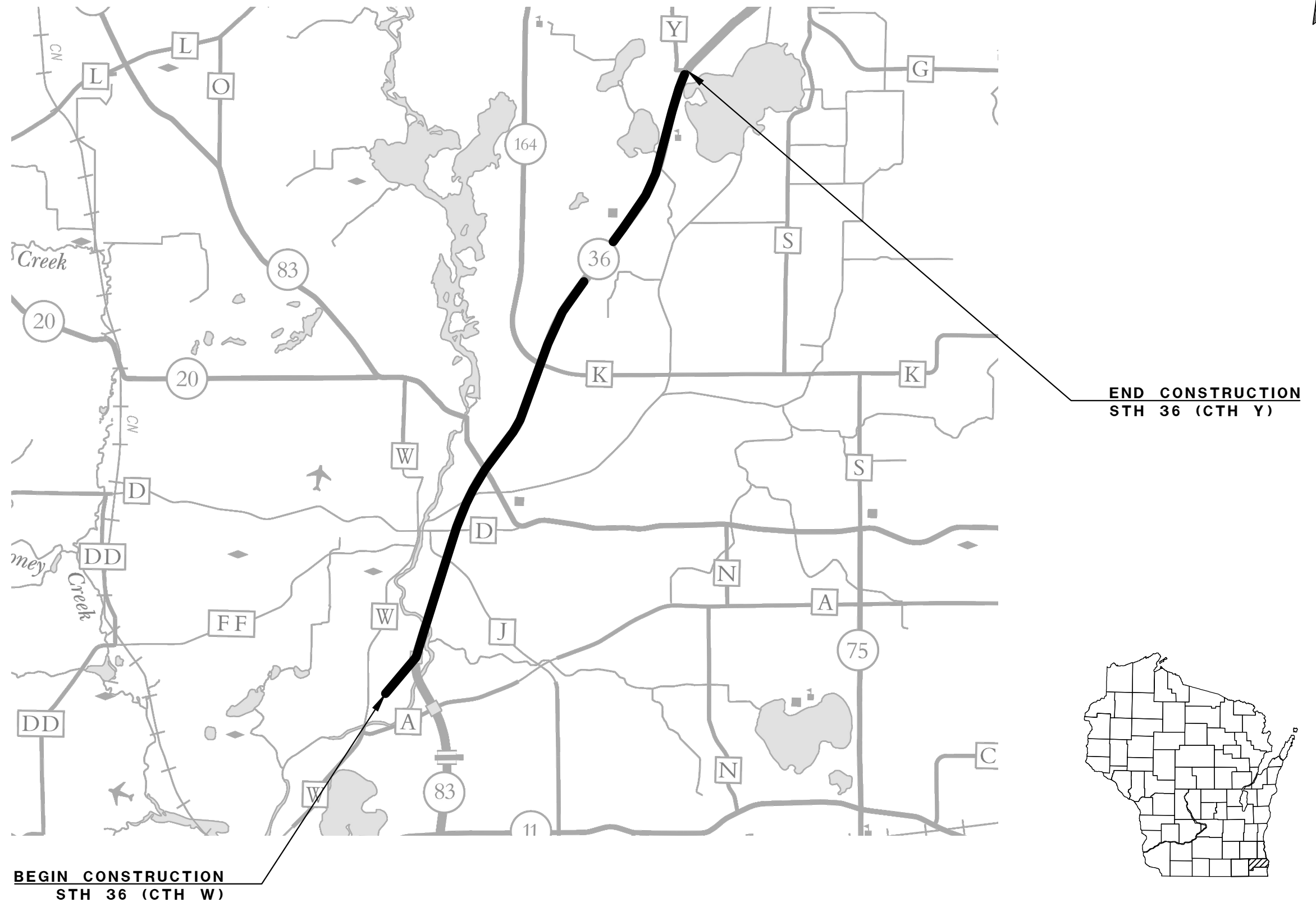
N



RACINE COUNTY

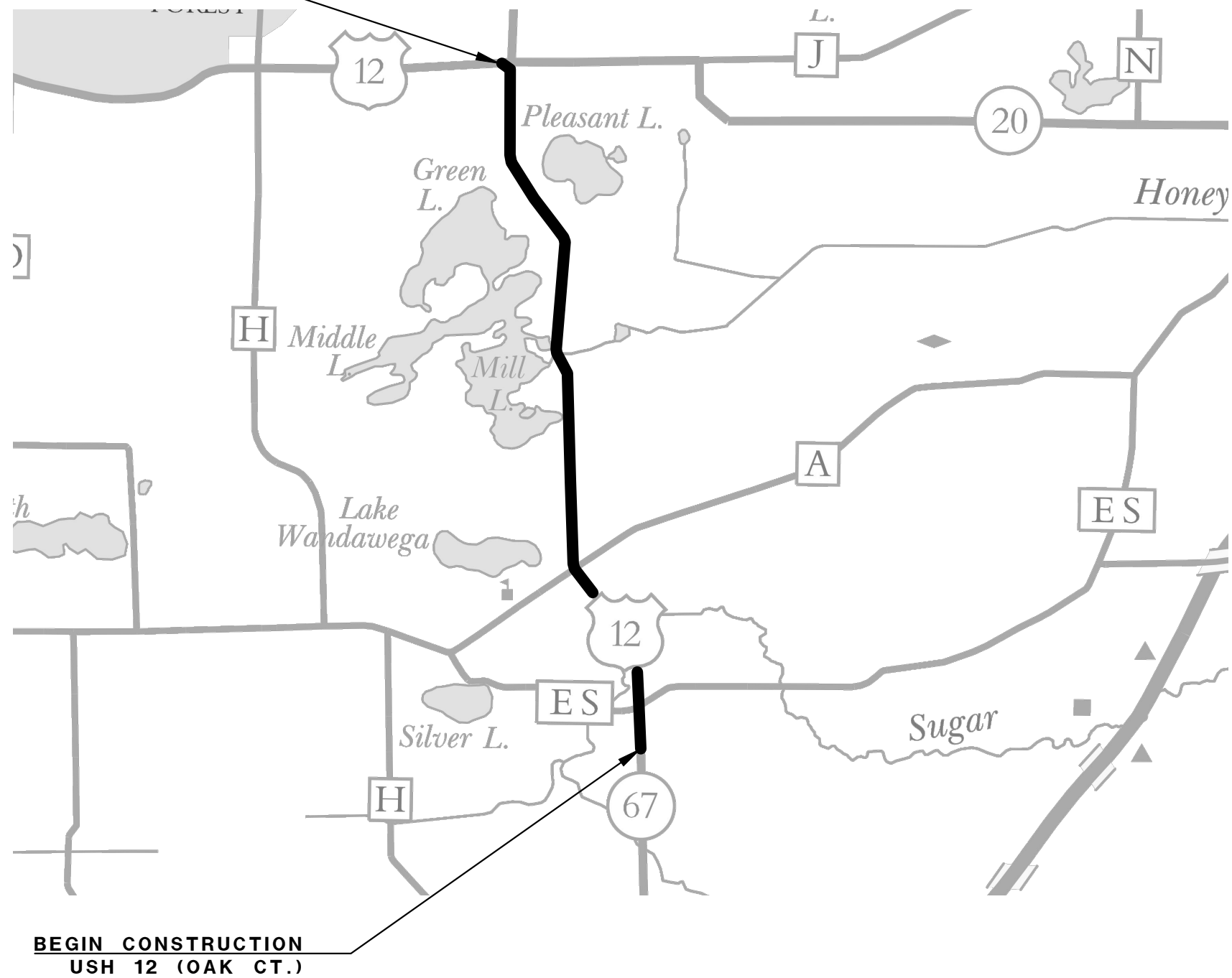


RACINE COUNTY



END CONSTRUCTION
USH 12 (STH 20)

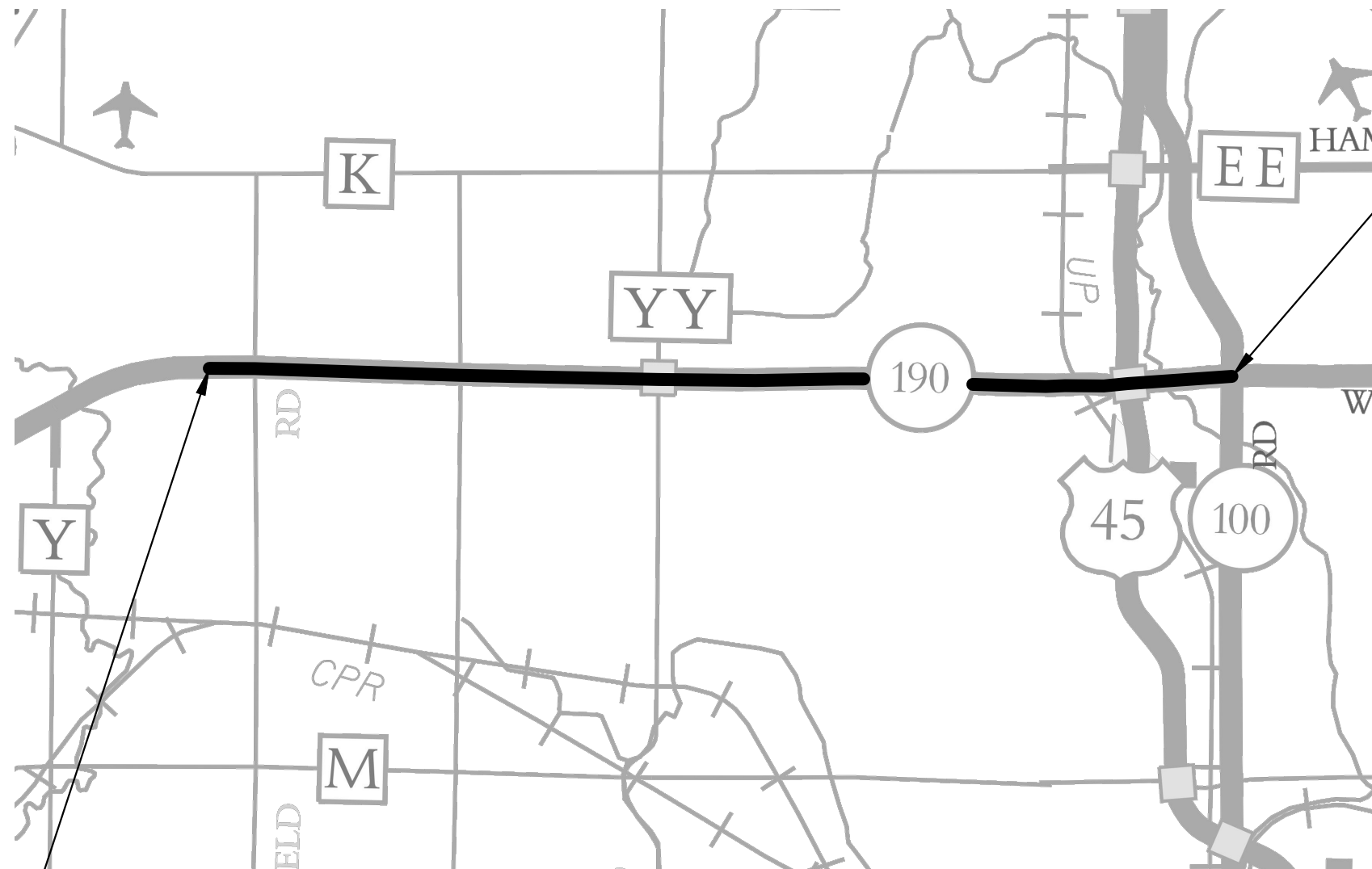
WALWORTH COUNTY



WAUKESHA COUNTY

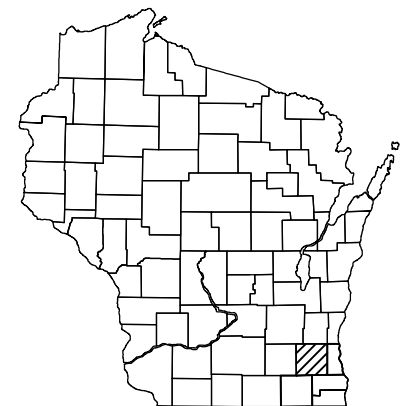


WAUKESHA COUNTY

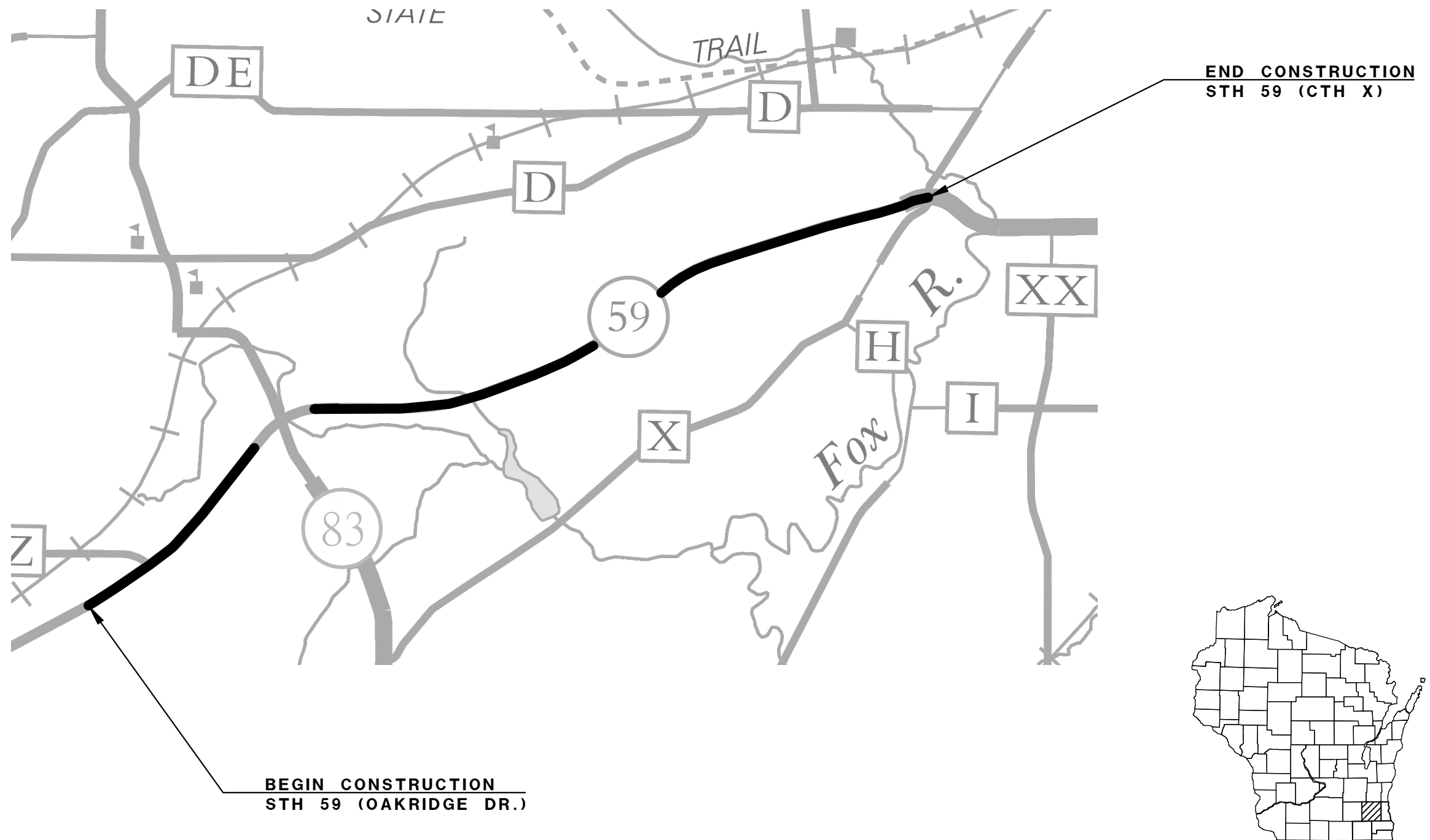


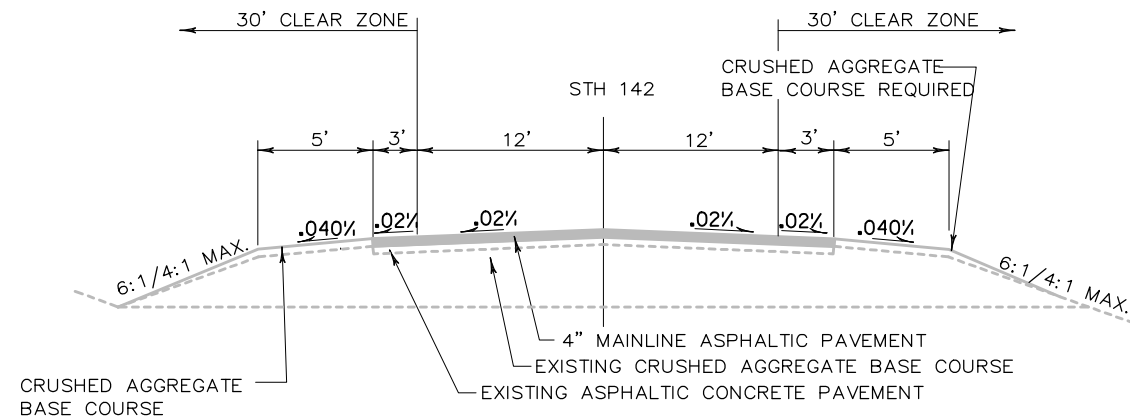
END CONSTRUCTION
STH 190 (STH 100)

BEGIN CONSTRUCTION
STH 190 (BROOKFIELD RD.)

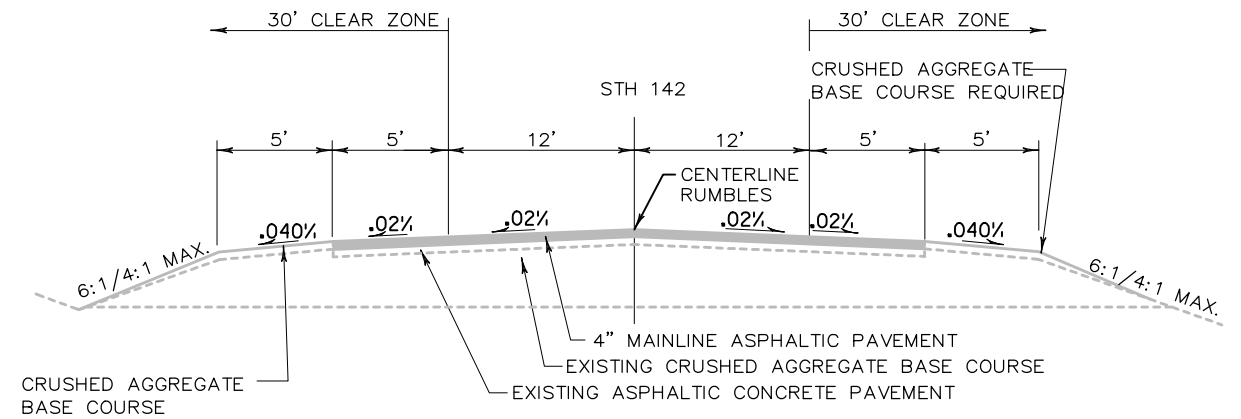


WAUKESHA COUNTY

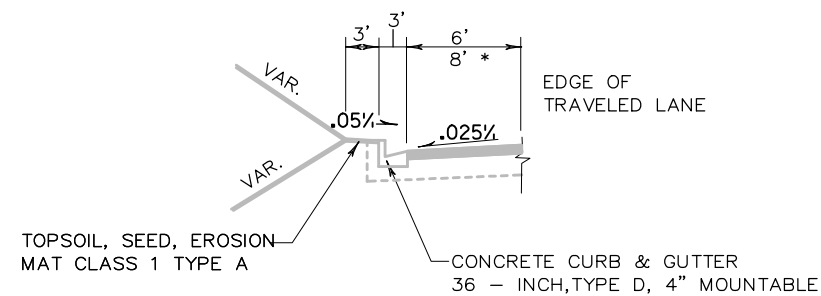
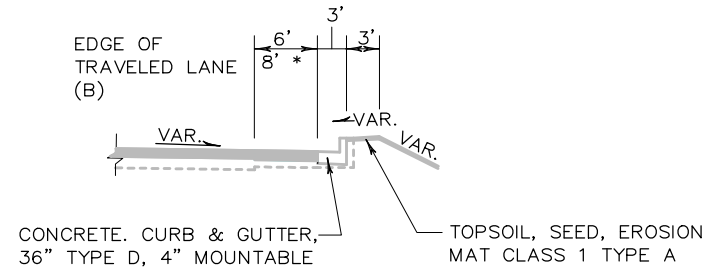
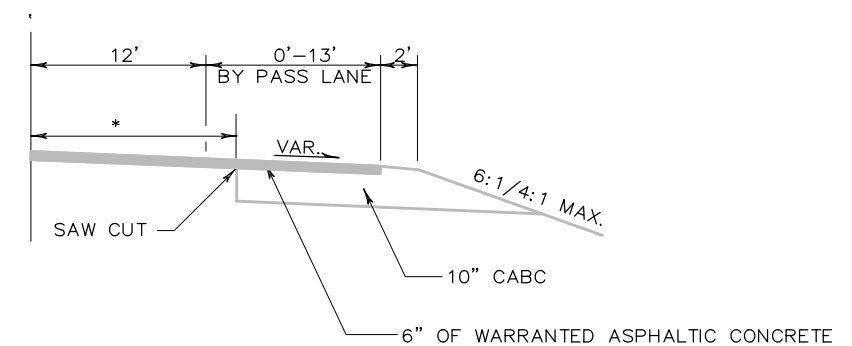




EXISTING TYPICAL SECTION

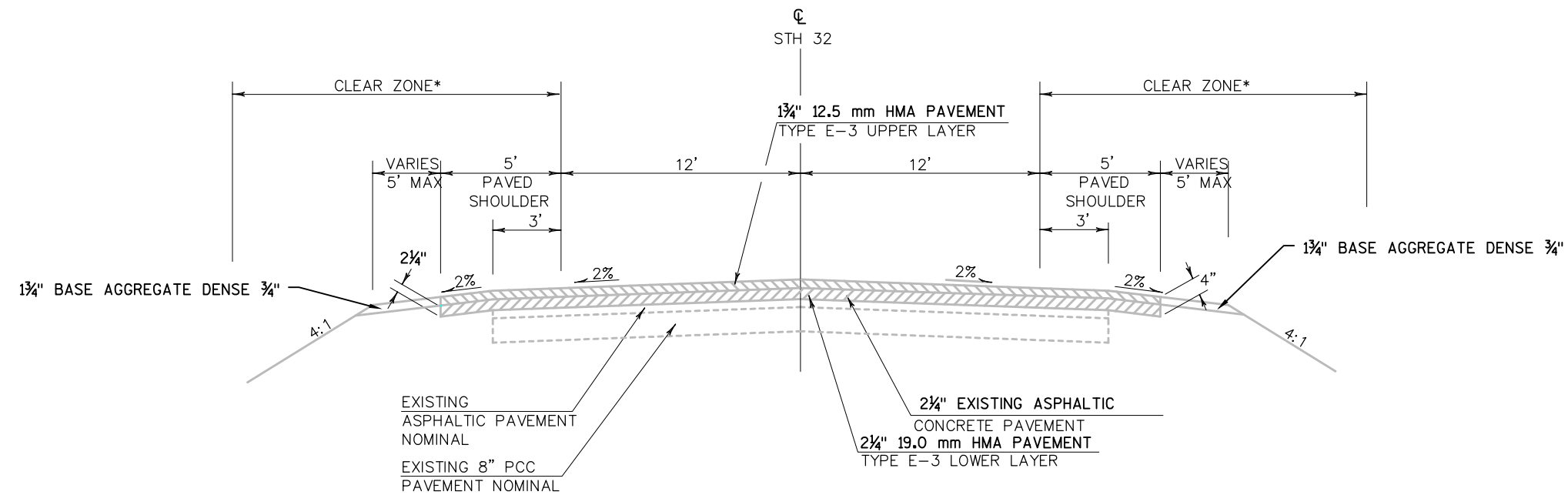
STH 142
CTY J TO STH 75

EXISTING TYPICAL SECTION

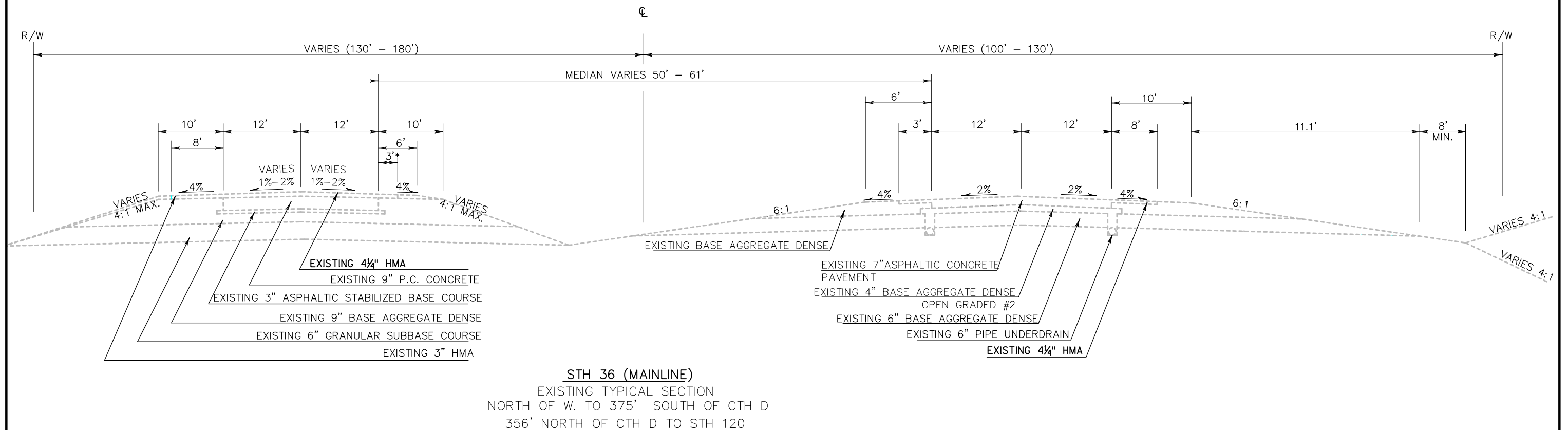
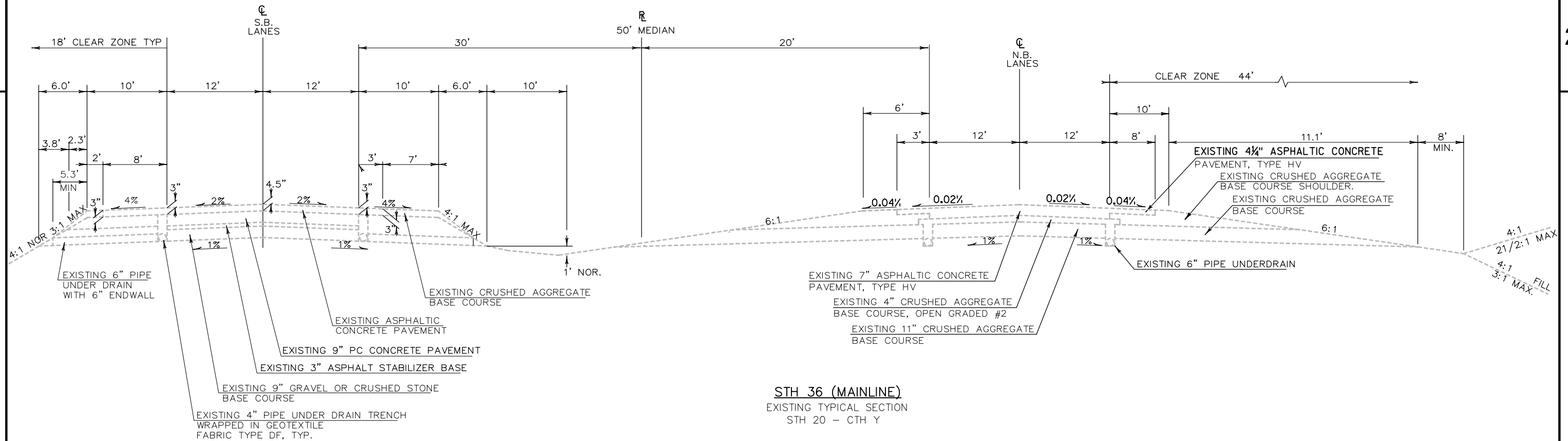
STH 142
STH 75 TO IH-94EXISTING TYPICAL SECTION
BRIGHTON CREEK
550' WEST OF 216TH AVE.
400' EAST OF CTH UEEXISTING TYPICAL SECTION
200' EAST OF CTH D

BY PASS LANE TYPICAL SECTION

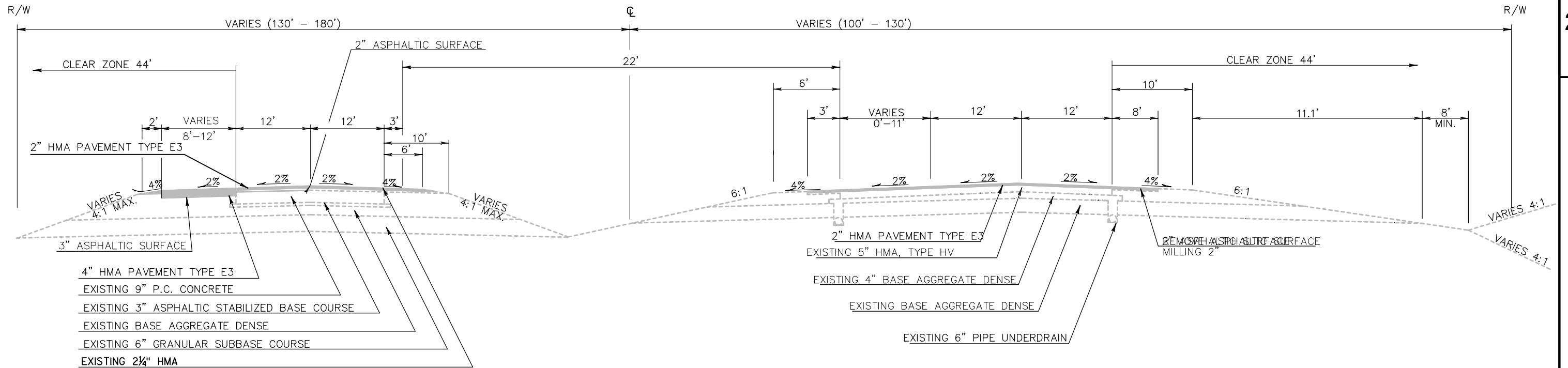
* CTH B
224TH AVE.
216TH AVE.



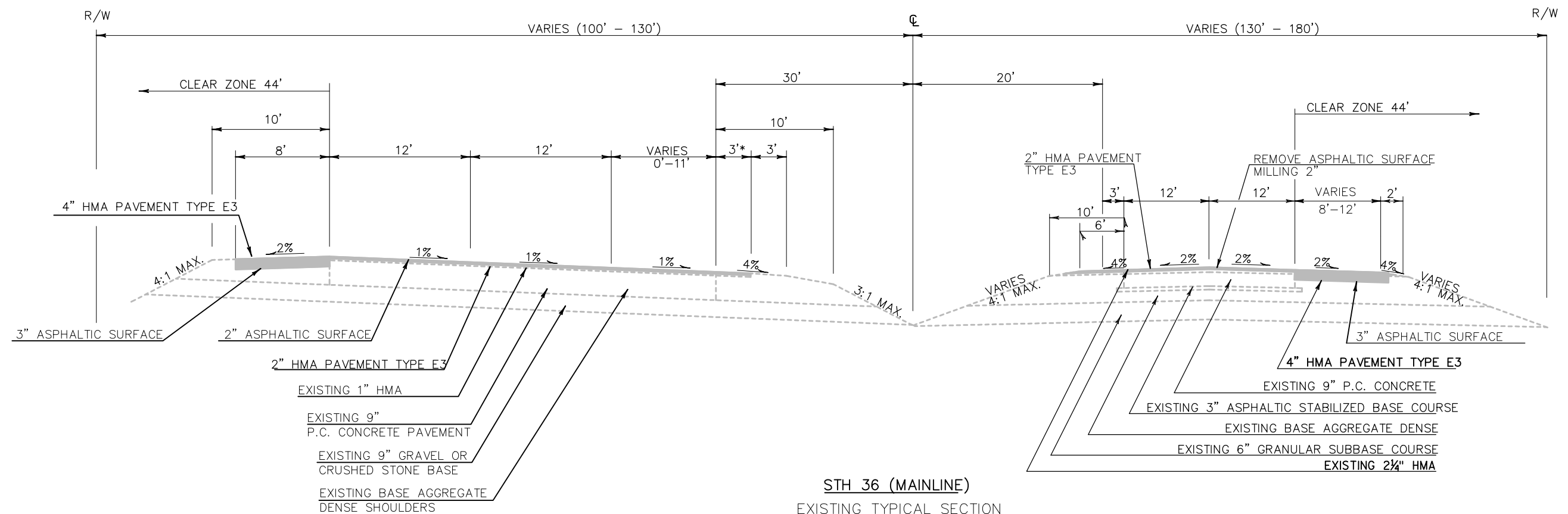
EXISTING TYPICAL SECTION
STH 32
GEHRING RD. TO FOREST HILL AVE.



2



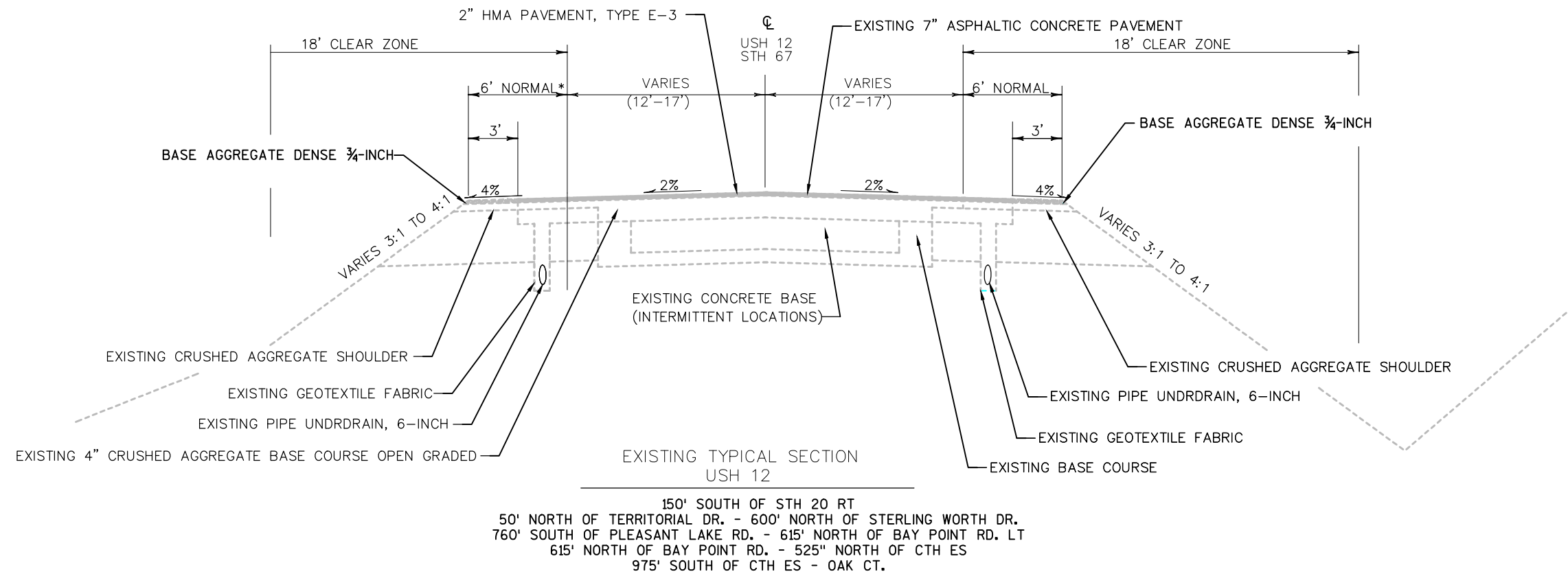
STH 36 (MAINLINE)
EXISTING TYPICAL SECTION
BEGIN 375' SOUTH OF CTH D INTERSECTION



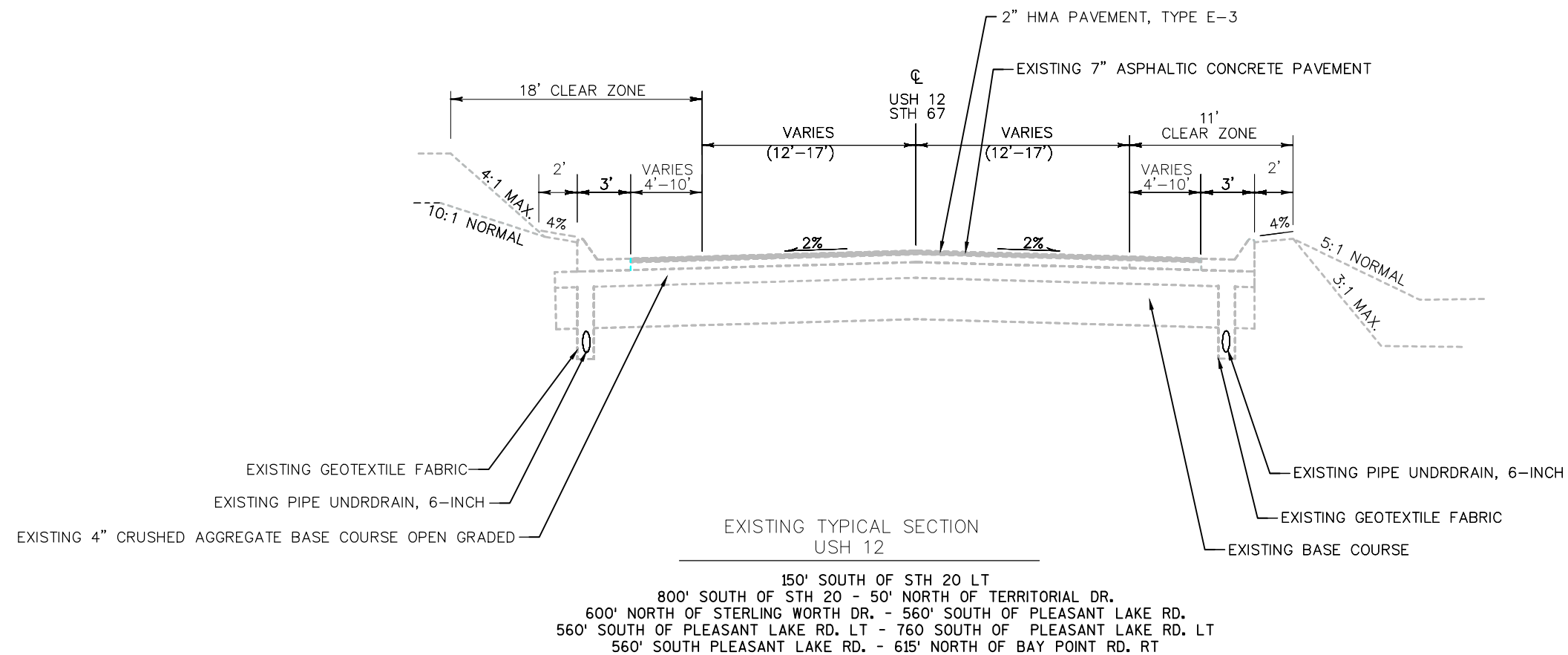
STH 36 (MAINLINE)
EXISTING TYPICAL SECTION
BEGIN 356' NORTH OF CTH D INTERSECTION

2

2 |



NOTE: CLEAR ZONE & SUPER ELEVATION CHANGES NOT SHOWN



PROJECT NO: 4890-00-71

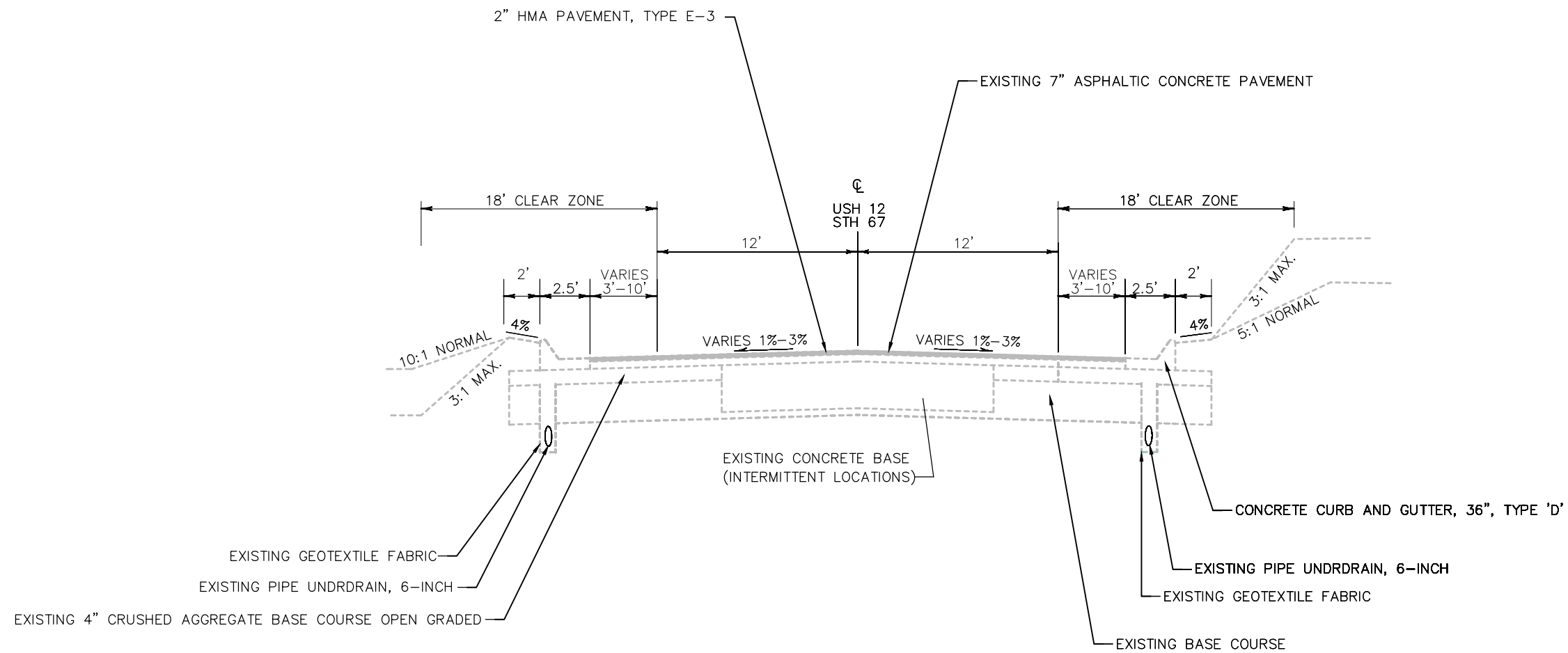
HWY: VARIOUS HIGHWAYS

COUNTY: WALWORTH

TYPICAL SECTIONS

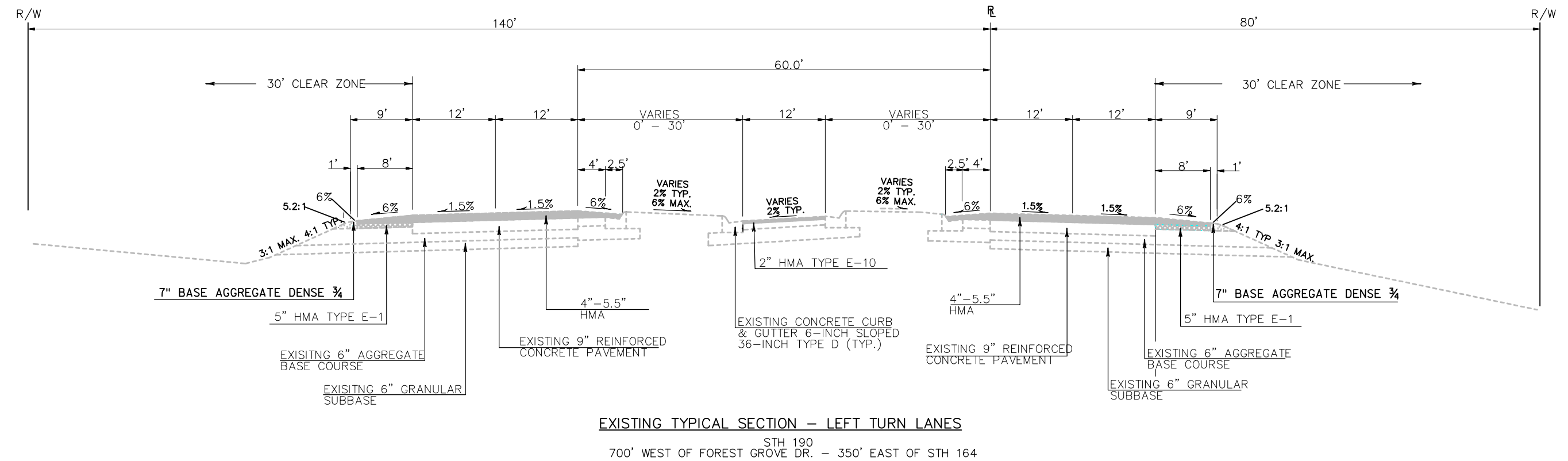
SHEET

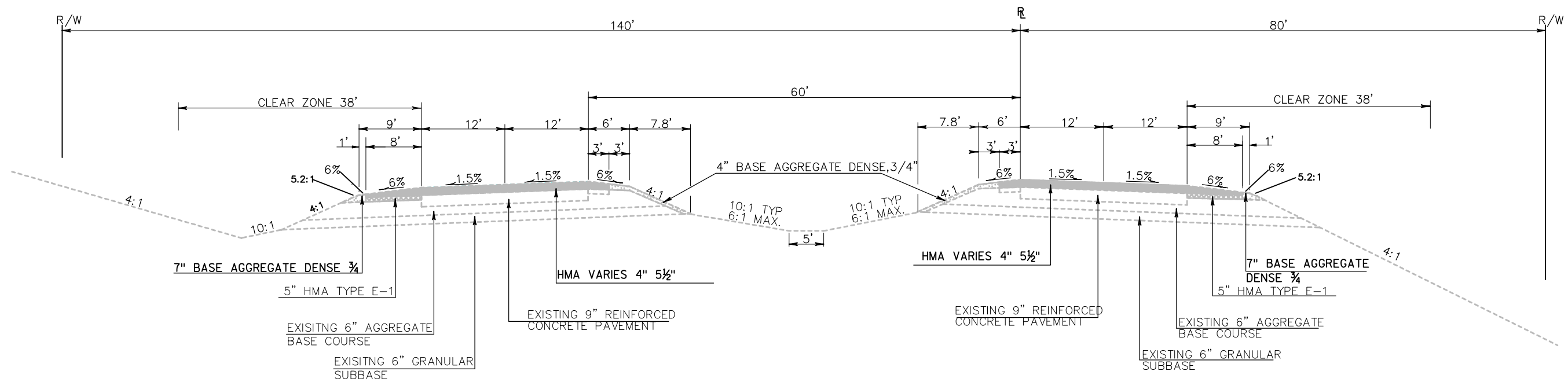
11



EXISTING TYPICAL URBAN SECTION
USH 12

525' NORTH OF CTH ES - 975' SOUTH OF CTH ES

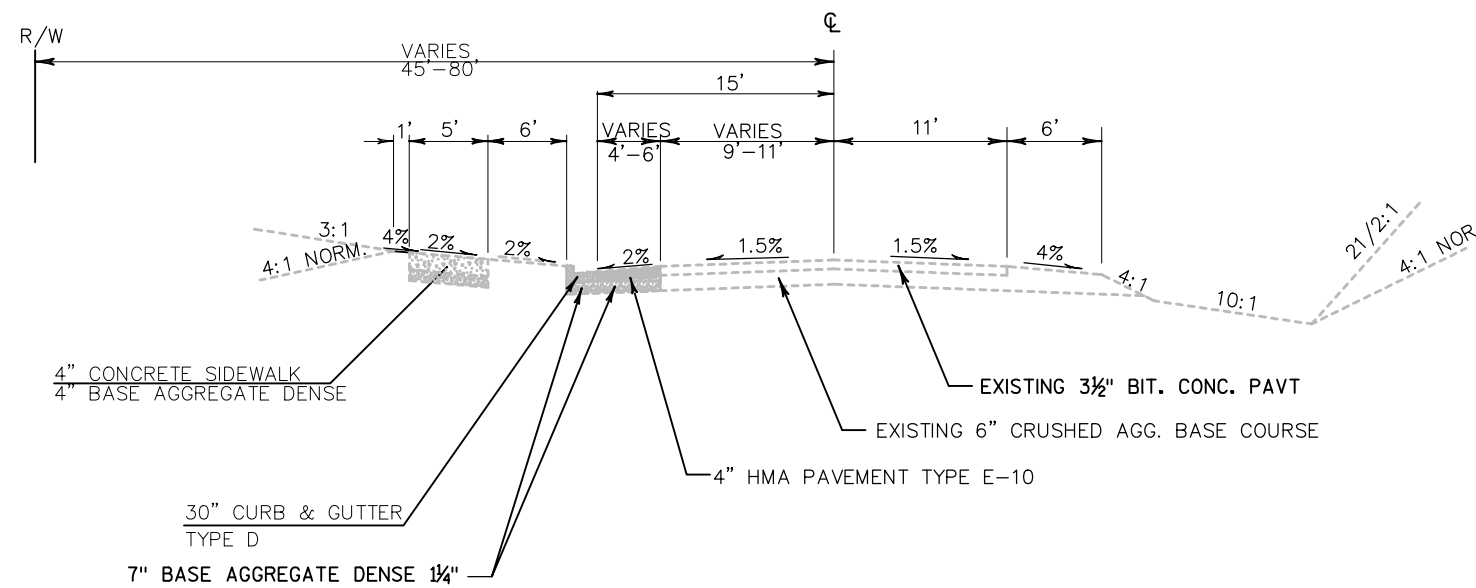




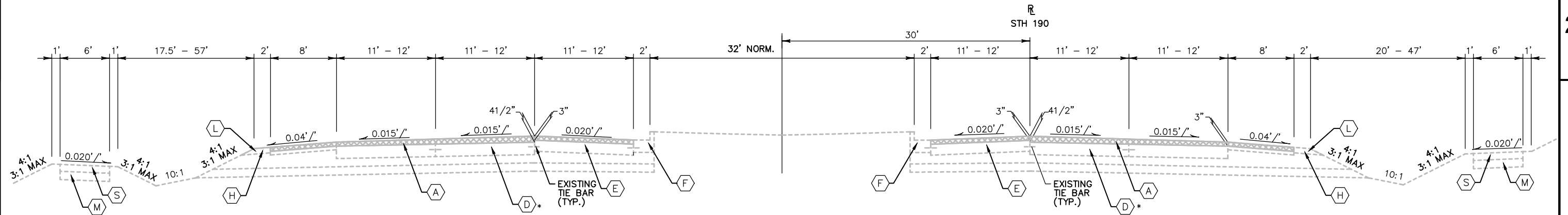
FINISHED TYPICAL SECTION

STH 190

350' EAST OF STH 164 - 150' EAST OF FIVE FIELDS RD.



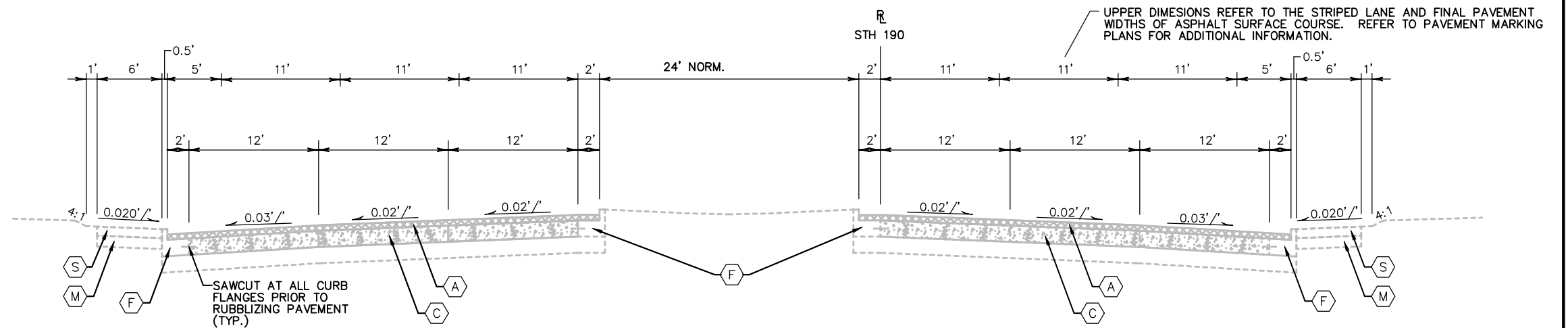
EXISTING TYPICAL SECTION
AT CECELIA DRIVE



LEGEND - TYPICAL FINISHED SECTION

- ** (A) 4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (B) 13/4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (C) RUBBLIZE EXISTING 7" - 8"
- * (D) EXISTING 9" P.C. REINFORCED CONCRETE
- (E) EXISTING 8" P.C. REINFORCED CONC.
- (F) EXISTING CURB & GUTTER
- (G) EXISTING CONCRETE WALK
- (H) SHAPING SHOULDERS
- (I) 4" HMA PAVEMENT, TYPE E-3
- (J) 61/4" HMA PAVEMENT, TYPE E-3
- (K) CONCRETE BASE 8-INCH
- (L) 3" MIN BASE AGGREGATE DENSE, 3/4-INCH (FDM 14-5-1)
- (M) 4" BASE AGGREGATE DENSE, 1 1/4-INCH
- (N) 6" BASE AGGREGATE DENSE, 1 1/4-INCH
- (O) 12" BASE AGGREGATE DENSE, 1 1/4-INCH
- (P) CONCRETE CURB & GUTTER 36-INCH
- (Q) CONCRETE CURB & GUTTER 30-INCH
- (R) 9" CONCRETE CORRUGATED MEDIAN
- (S) CONCRETE SIDEWALK 5-INCH
- (T) RETAINING WALL
- (U) RUBBLIZED EXISTING CONCRETE PAVEMENT

- * NOTE: EXISTING CONCRETE BASE SLOPE = 0.01'/'.
ASPHALT WEDGING WAS CONSTRUCTED TO
OBTAIN 0.015'/' SURFACE CROSS SLOPE.
- ** NOTE: ASPHALT WEDGING EXISTS WITHIN THE PROJECT CORRIDOR.
CONTRACTOR SHALL REMOVE THE TOP 4" OF PAVEMENT, WHICH
MAY OR MAY NOT INCLUDE APPROXIMATELY 1" REMOVING
CONCRETE SURFACE PARTIAL DEPTH. EXISTING ASPHALT
THICKNESS VARIES BETWEEN 3" AND 4 1/2" DUE TO WEDGING.

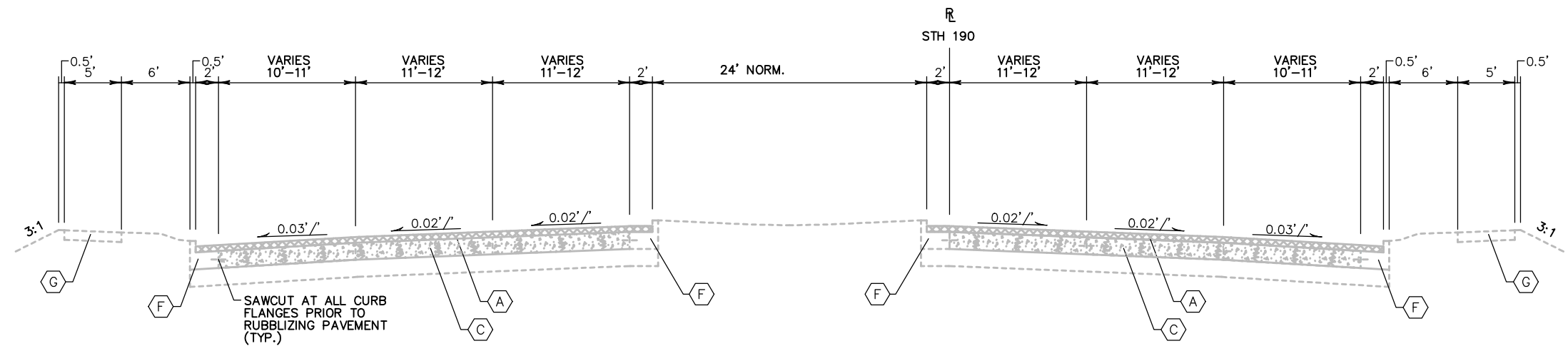


LEGEND — TYPICAL FINISHED SECTION

- ** (A) 4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (B) 1 3/4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (C) RUBBLIZE EXISTING 7" — 8"
- * (D) EXISTING 9" P.C. REINFORCED CONCRETE
- (E) EXISTING 8" P.C. REINFORCED CONC.
- (F) EXISTING CURB & GUTTER
- (G) EXISTING CONCRETE WALK
- (H) SHAPING SHOULDERS
- (I) 4" HMA PAVEMENT, TYPE E-3
- (J) 6 1/4" HMA PAVEMENT, TYPE E-3
- (K) CONCRETE BASE 8-INCH
- (L) 3" MIN BASE AGGREGATE DENSE, 3/4-INCH (FDM 14-5-1)
- (M) 4" BASE AGGREGATE DENSE, 1 1/4-INCH
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- (T) RETAINING WALL
- (U) RUBBLIZED EXISTING CONCRETE PAVEMENT

* NOTE: EXISTING CONCRETE BASE SLOPE = 0.01'/'.
ASPHALT WEDGING WAS CONSTRUCTED TO
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CONCRETE SURFACE PARTIAL DEPTH. EXISTING ASPHALT
THICKNESS VARIES BETWEEN 3" AND 4 1/2" DUE TO WEDGING.

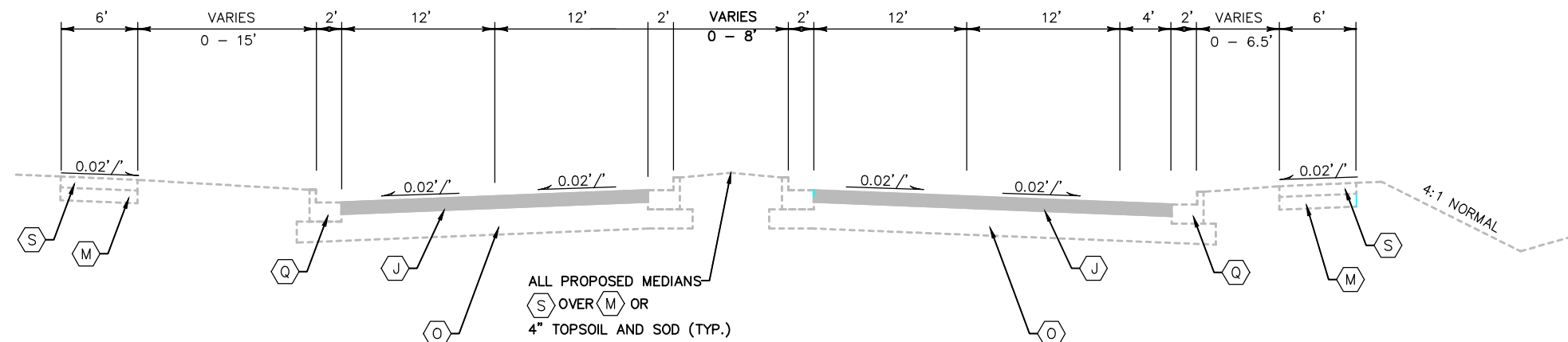
EXISTING TYPICAL SECTIONSTH 190 — MAINLINE

AT FIVE FIELDS RD.

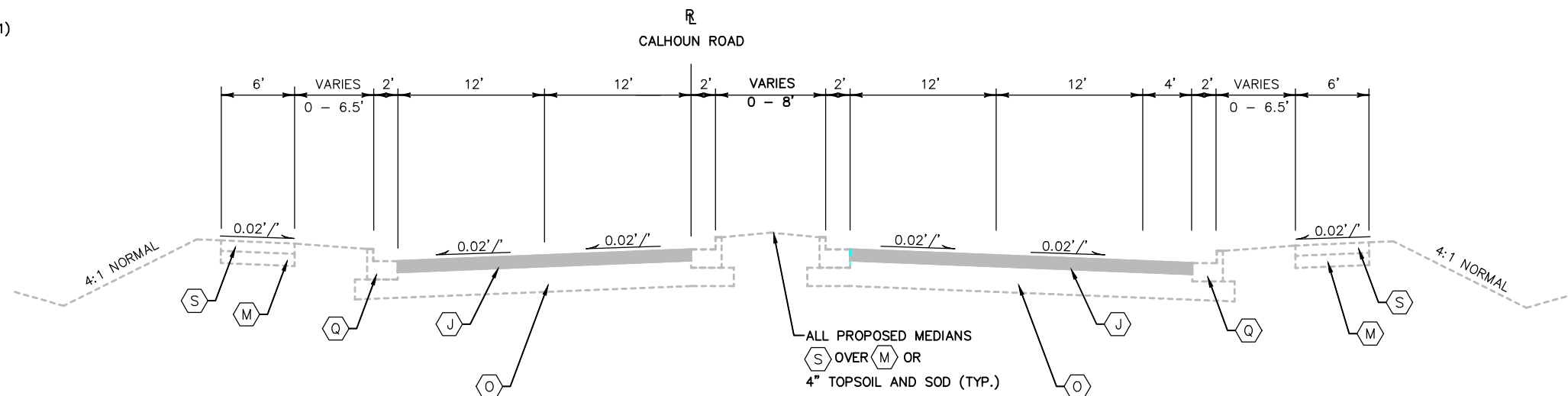
LEGEND — TYPICAL FINISHED SECTION

- ** (A) 4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (B) 13/4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (C) RUBBLIZE EXISTING 7" - 8"
- * (D) EXISTING 9" P.C. REINFORCED CONCRETE
- (E) EXISTING 8" P.C. REINFORCED CONC.
- (F) EXISTING CURB & GUTTER
- (G) EXISTING CONCRETE WALK
- (H) SHAPING SHOULDERS
- (I) 4" HMA PAVEMENT, TYPE E-3
- (J) 61/4" HMA PAVEMENT, TYPE E-3
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- (O) 12" BASE AGGREGATE DENSE, 1 1/4-INCH
- (P) CONCRETE CURB & GUTTER 36-INCH
- (Q) CONCRETE CURB & GUTTER 30-INCH
- (R) 9" CONCRETE CORRUGATED MEDIAN
- (S) CONCRETE SIDEWALK 5-INCH
- (T) RETAINING WALL
- (U) RUBBLIZED EXISTING CONCRETE PAVEMENT

- * NOTE: EXISTING CONCRETE BASE SLOPE = 0.01'/'.
- ASPHALT WEDGING WAS CONSTRUCTED TO OBTAIN 0.015'/' SURFACE CROSS SLOPE.
- ** NOTE: ASPHALT WEDGING EXISTS WITHIN THE PROJECT CORRIDOR. CONTRACTOR SHALL REMOVE THE TOP 4" OF PAVEMENT, WHICH MAY OR MAY NOT INCLUDE APPROXIMATELY 1" REMOVING CONCRETE SURFACE PARTIAL DEPTH. EXISTING ASPHALT THICKNESS VARIES BETWEEN 3" AND 4 1/2" DUE TO WEDGING.



EXISTING TYPICAL SECTION
AT CALHOUN ROAD

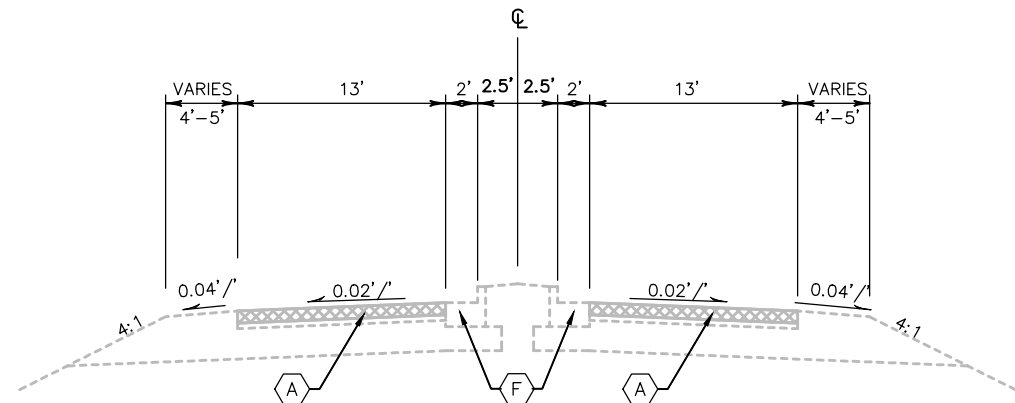


EXISTING TYPICAL SECTION
AT CALHOUN ROAD

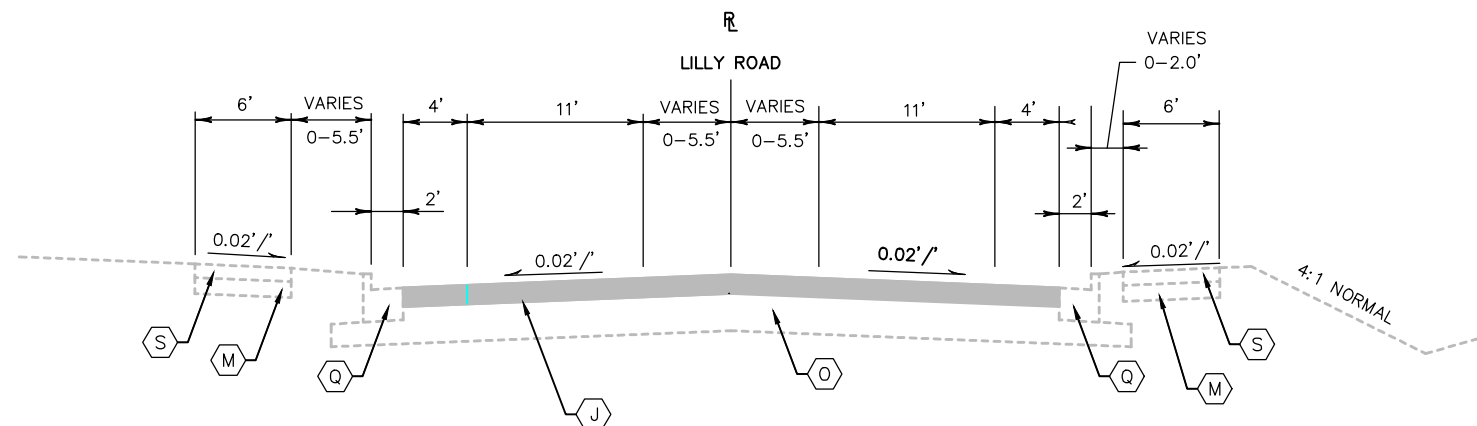
LEGEND — TYPICAL FINISHED SECTION

- ** (A) 4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (B) 13/4" HMA PAVEMENT, TYPE E-3 OVERLAY
- (C) RUBBLIZE EXISTING 7" - 8"
- * (D) EXISTING 9" P.C. REINFORCED CONCRETE
- (E) EXISTING 8" P.C. REINFORCED CONC.
- (F) EXISTING CURB & GUTTER
- (G) EXISTING CONCRETE WALK
- (H) SHAPING SHOULDERS
- (I) 4" HMA PAVEMENT, TYPE E-3
- (J) 61/4" HMA PAVEMENT, TYPE E-3
- (K) CONCRETE BASE 8-INCH
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- (O) 12" BASE AGGREGATE DENSE, 1 1/4-INCH
- (P) CONCRETE CURB & GUTTER 36-INCH
- (Q) CONCRETE CURB & GUTTER 30-INCH
- (R) 9" CONCRETE CORRUGATED MEDIAN
- (S) CONCRETE SIDEWALK 5-INCH
- (T) RETAINING WALL
- (U) RUBBLIZED EXISTING CONCRETE PAVEMENT

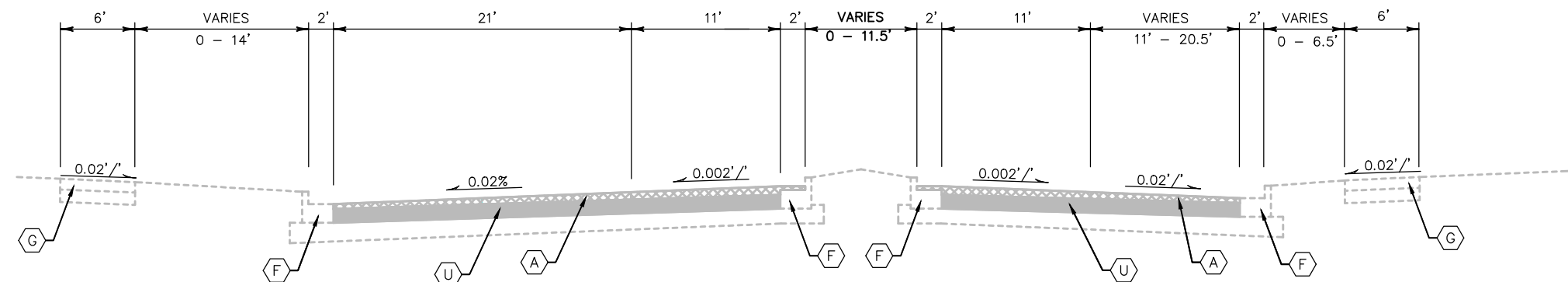
- * NOTE: EXISTING CONCRETE BASE SLOPE = 0.01'/'.
- ASPHALT WEDGING WAS CONSTRUCTED TO OBTAIN 0.015'/' SURFACE CROSS SLOPE.
- ** NOTE: ASPHALT WEDGING EXISTS WITHIN THE PROJECT CORRIDOR. CONTRACTOR SHALL REMOVE THE TOP 4" OF PAVEMENT, WHICH MAY OR MAY NOT INCLUDE APPROXIMATELY 1" REMOVING CONCRETE SURFACE PARTIAL DEPTH. EXISTING ASPHALT THICKNESS VARIES BETWEEN 3" AND 4 1/2" DUE TO WEDGING.



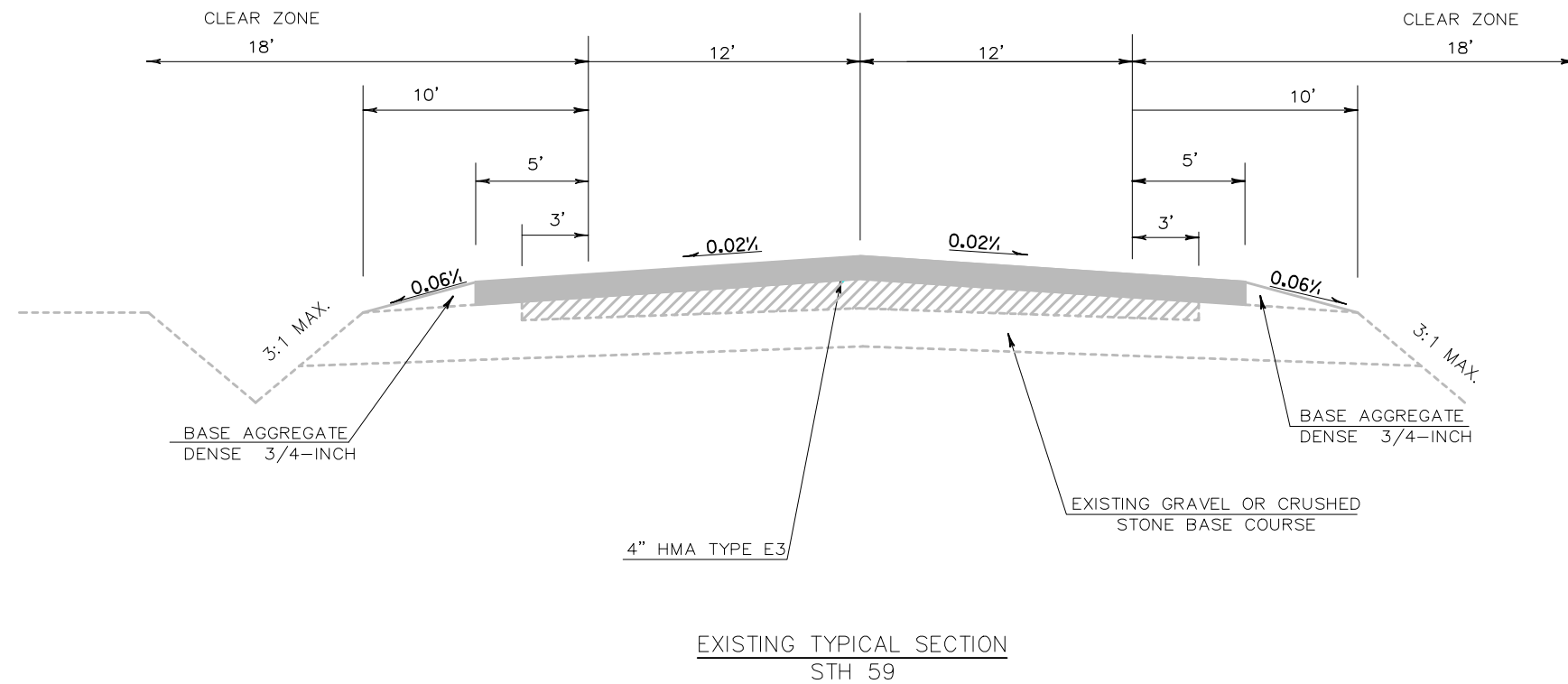
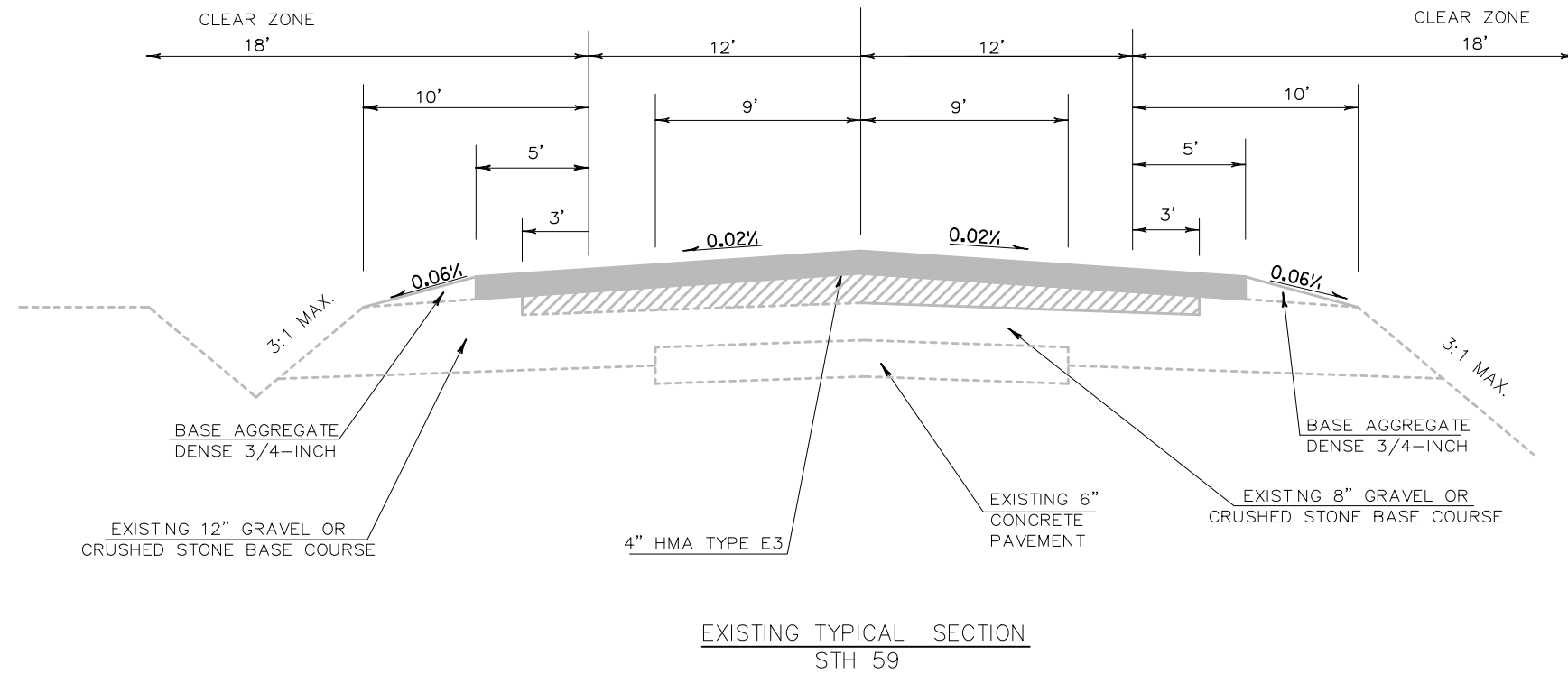
EXISTING TYPICAL SECTION
AT PILGRIM ROAD

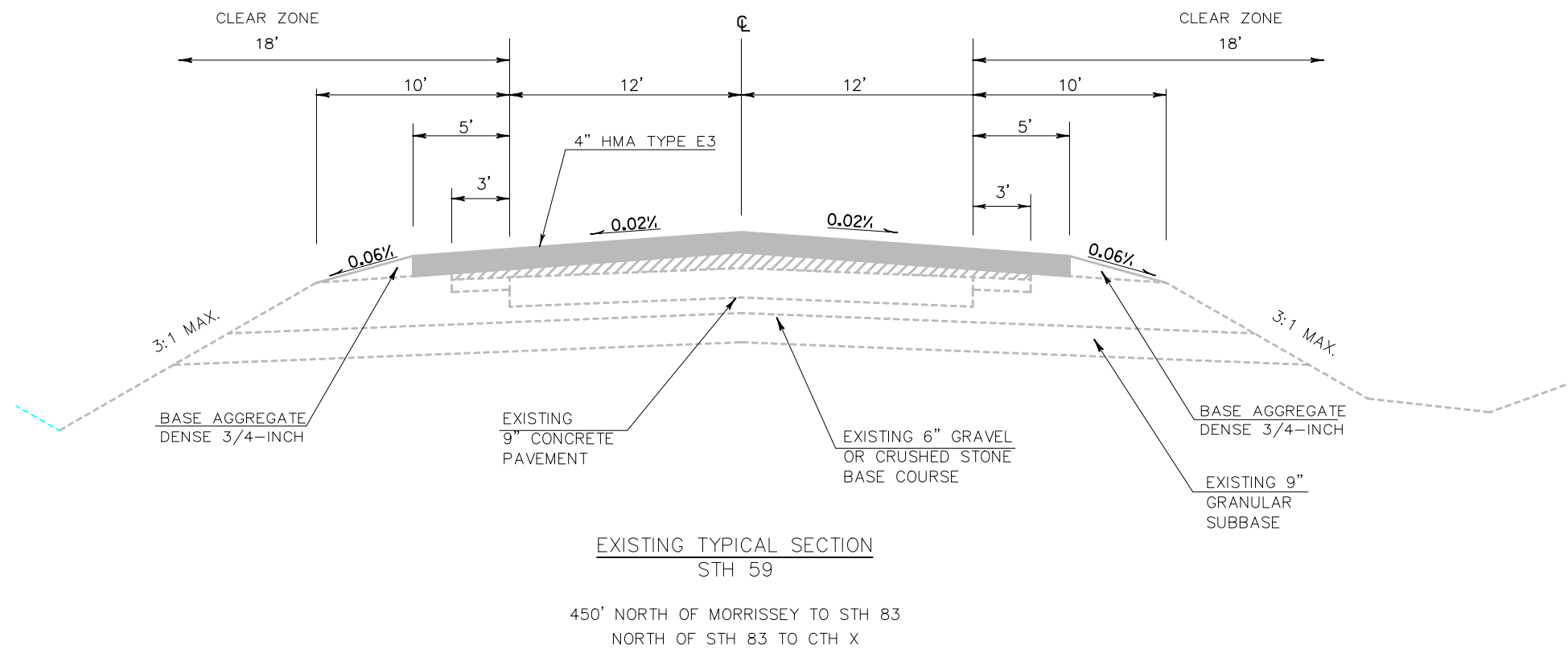


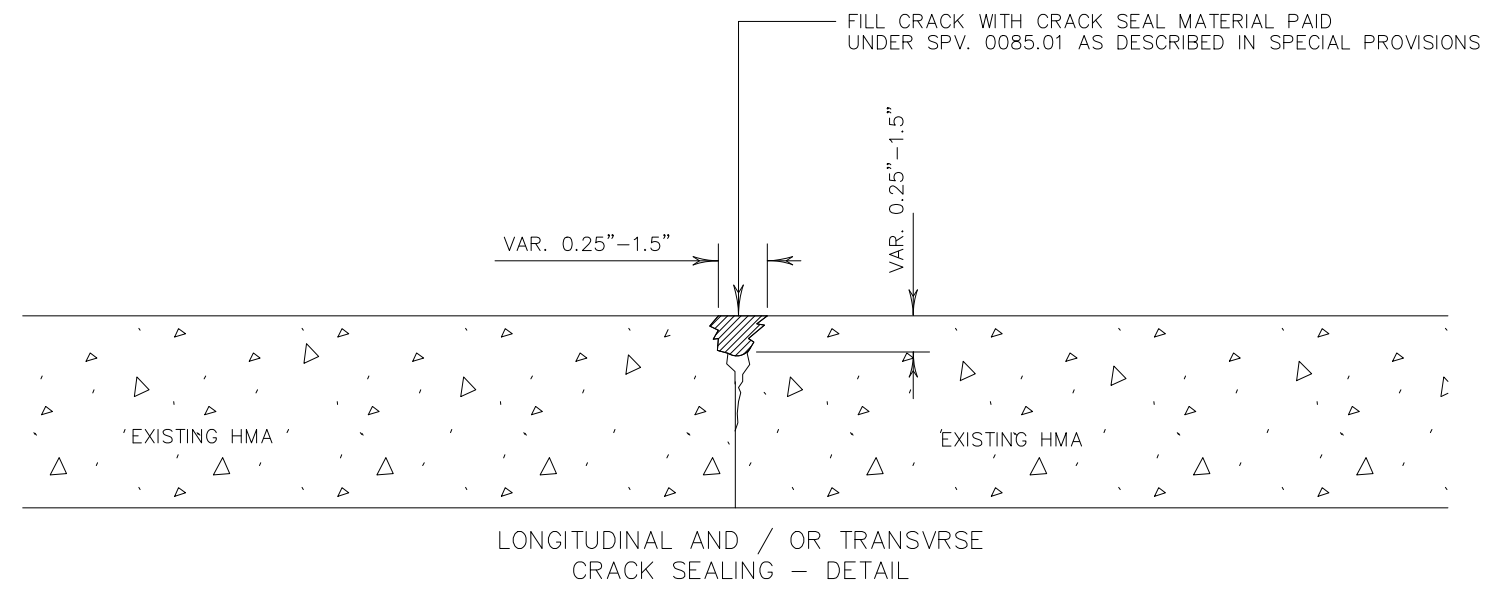
EXISTING TYPICAL SECTION
AT LILLY ROAD



EXISTING TYPICAL SECTION
AT 124TH STREET







DATE 05MAY15		E S T I M A T E O F Q U A N T I T I E S			
LINE					4890-00-71
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	213.0100	Finishing Roadway (project) 01. 4890-00-71	EACH	1.000	1.000
0020	619.1000	Mobilization	EACH	1.000	1.000
0030	643.0100	Traffic Control (project) 01. 4890-00-71	EACH	1.000	1.000
0040	643.0300	Traffic Control Drums	DAY	2,415.000	2,415.000
0050	643.0420	Traffic Control Barricades Type III	DAY	178.000	178.000
0060	643.0705	Traffic Control Warning Lights Type A	DAY	237.000	237.000
0070	643.0800	Traffic Control Arrow Boards	DAY	33.000	33.000
0080	643.0900	Traffic Control Signs	DAY	262.000	262.000
0090	643.1050	Traffic Control Signs PCMS	DAY	8.000	8.000
0100	646.0106	Pavement Marking Epoxy 4-Inch	LF	782,522.000	782,522.000
0110	646.0126	Pavement Marking Epoxy 8-Inch	LF	350.000	350.000
0120	SPV.0085	Special 01. CRACK SEALING MODIFIED	LB	130,791.000	130,791.000
0130	SPV.0085	Special 02. ROUT AND SEAL MODIFIED	LB	108,893.000	108,893.000

CRACK SEALING

LIMIT		LIMIT	HIGHWAY	NUMBER OF LANES	LENGTH (LF)	SPV.0085.01 CRACK SEALING MODIFIED (LB)	SPV.0085.02 ROUT AND SEAL MODIFIED (LB)
GEHRING RD	-	RACINE CO. LINE	STH 32	2	12,642	--	4,214.00
RACINE CO. LINE	-	FOREST HILL AVE	STH 32	2	39,928	9,982	--
CTH W	-	STH 20	STH 36	4	67,126	--	22,375
STH 20	-	STH 164	STH 36	4	65,575	--	21,858
STH 164	-	CTH Y	STH 36	4	181,335	--	60,445
N. PRAIRIE	-	STH 83	STH 59	2	27,352	6,838	--
STH 83	-	CTH X	STH 59	2	52,371	13,093	--
OAK CT	-	STH 20	STH 67	2	105,348	26,337	--
CTH J	-	I 94	STH 142	2	109,958	27,490	--
STH 16	-	5 FIELDS DR	STH 190	4	59,122	14,781	--
BROOKFIELD RD	-	124TH	STH 190	6	102,564	25,641	--
124TH		STH 100	STH 190	6	26,520	6,630	--
TOTAL					849,841	130,791	108,893

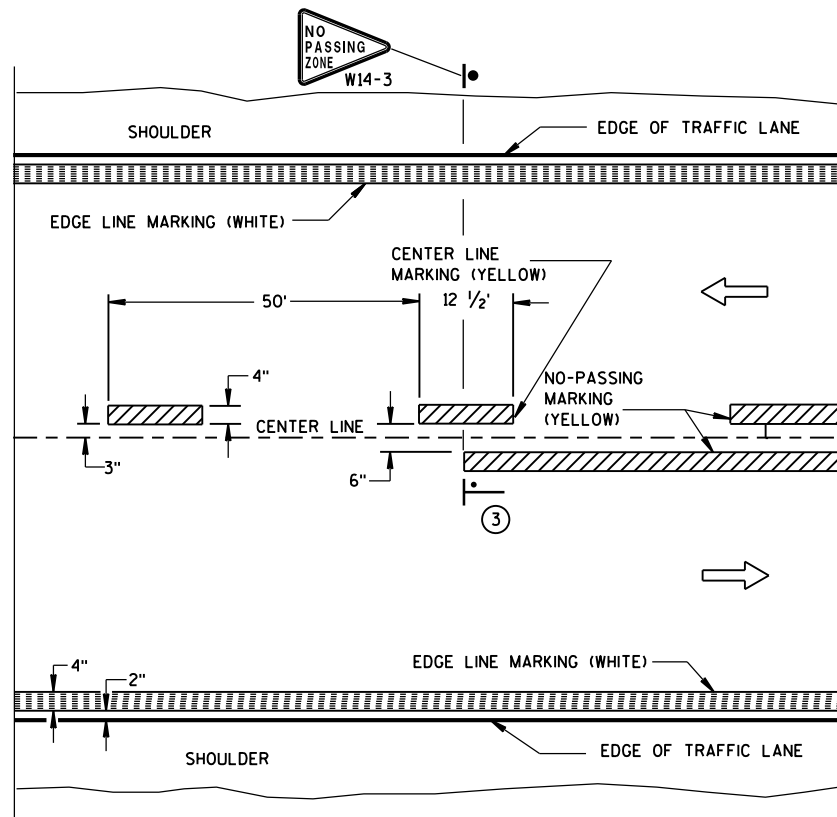
TRAFFIC CONTROL

LIMIT	-	LIMIT	HIGHWAY	LANES BEING CLOSED	PROJECT LENGTH (LF)	APPROX. DAYS CLOSED	643.0300 DRUMS (DAY)	643.0420 BARRICADES TYPE III (DAY)	643.0715 WARNING LIGHTS TYPE C (DAY)	643.0800 ARROW BOARDS (DAY)	643.0900 SIGNS (DAY)	643.1050 SIGNS PCMS
GEHRING RD	-	FOREST HILL AVE	STH 32	1	27,920	1	--	--	--	--	8	--
CTH W	-	CTH Y	STH 36	1	47,450	6	1,200	72	84	12	66	--
N. PRAIRIE	-	CTH X	STH 59	1	36,821	3	--	--	--	--	24	--
OAK CT	-	STH 20	STH 67	1	31,350	3	--	--	--	--	24	--
CTH J	-	I 94	STH 142	1	64,475	4	--	--	--	--	32	--
STH 16	-	5 FIELDS DR	STH 190	1	10,214	3	315	21	33	6	33	3
BROOKFIELD RD	-	STH 100	STH 190	2	27,075	5	900	85	120	15	75	5
TOTALS						25	2415	178	237	33	262	8

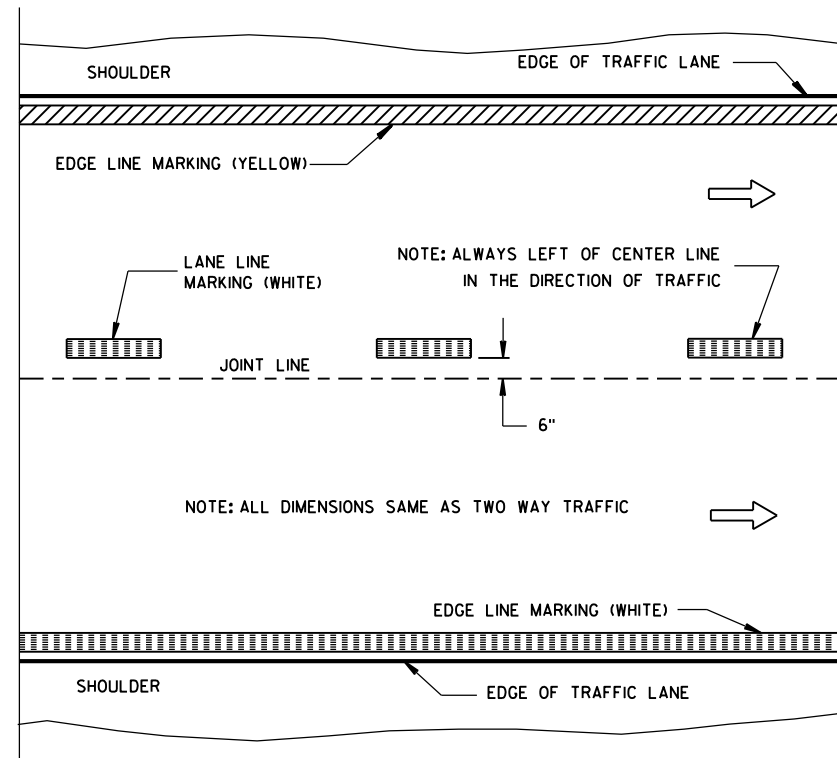
PAVEMENT MARKING ITEMS									
LIMIT		LIMIT	HIGHWAY	NUMBER OF LANES	646.0106 PAVEMENT MARKING EPOXY 4-INCH (SOLID WHITE) LF	646.0106 PAVEMENT MARKING EPOXY 4-INCH (DASHED WHITE) LF	646.0106 PAVEMENT MARKING EPOXY 4-INCH (SOLID YELLOW) LF	646.0106 PAVEMENT MARKING EPOXY 4-INCH (DASHED YELLOW) LF	646.0126 PAVEMENT MARKING EPOXY 8-INCH (SOLID WHITE) LF
GEHRING RD	--	RACING CO. LINE	STH 32	2	4,515	--	4,515	2140	
RACINE CO. LINE	--	FOREST HILL AVE	STH 32	2	14,260	90	14,260	6,300	350
CTH W	--	STH 20	STH 36	4	21,550	5,172	21,550	--	--
STH 20	--	STH 164	STH 36	4	15,250	3,660	15,250	--	--
STH 164	--	CTH Y	STH 36	4	47,100	11,304	47,100	--	--
N. PRAIRIE	--	STH 83	STH 59	2	23,180	--	11,590	1,159	--
STH 83		CTH X	STH 59	2	46,346	--	20,856	2,086	--
OAK CT	--	STH 20	STH 67	2	62,700	--	62,700	6,270	--
CTH J	--	I 94	STH 142	2	128,950	--	64,475	6,448	--
STH 16	--	5 FIELDS DR	STH 190	4	10,094	10,214	10,214	--	--
BROOKFIELD RD	--	124TH	STH 190	6	22,200	22,200	22,200	--	--
124TH	--	STH 100	STH 190	6	4,875	4,875	4,875	--	--
SUB-TOTAL					401,020	57,515	299,585	24,402	350
TOTALS					782,522				350

Standard Detail Drawing List

15C08-16A	PAVEMENT MARKING (MAINLINE)
15C08-16B	PAVEMENT MARKING (INTERSECTIONS)
15C08-16E	PAVEMENT MARKING (LEFT TURN LANE)
15C08-16F	PAVEMENT MARKING (ISLANDS)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C19-02A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C19-02B	MOVING PAVEMENT MARKING OPERATION MULTI-LANE UNDIVIDED ROADWAY
15C19-02C	MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY
15C21-05	SIGNING AND MARKING FOR TWO LANE TO FOUR LANE DIVIDED TRANSITIONS
15C31-01A	PAVEMENT MARKING (RAMPS AND GORES)
15C31-01B	LANE DROP PAVEMENT MARKING
15C31-01C	PAVEMENT MARKING MAJOR SPLIT FREEWAY TO FREEWAY
15C31-01D	PAVEMENT MARKING FOR PARALLEL ON-RAMP AND PARALLEL OFF-RAMP
15D12-03	TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M. P. H.
15D16-02	TRAFFIC CONTROL, EXIT RAMP CLOSURE
15D20-02	TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D21-02	TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE
15D22-02	TRAFFIC CONTROL, TWO LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
15D23-02	TRAFFIC CONTROL, INTERSECTION WITHIN TWO LANE CLOSURE

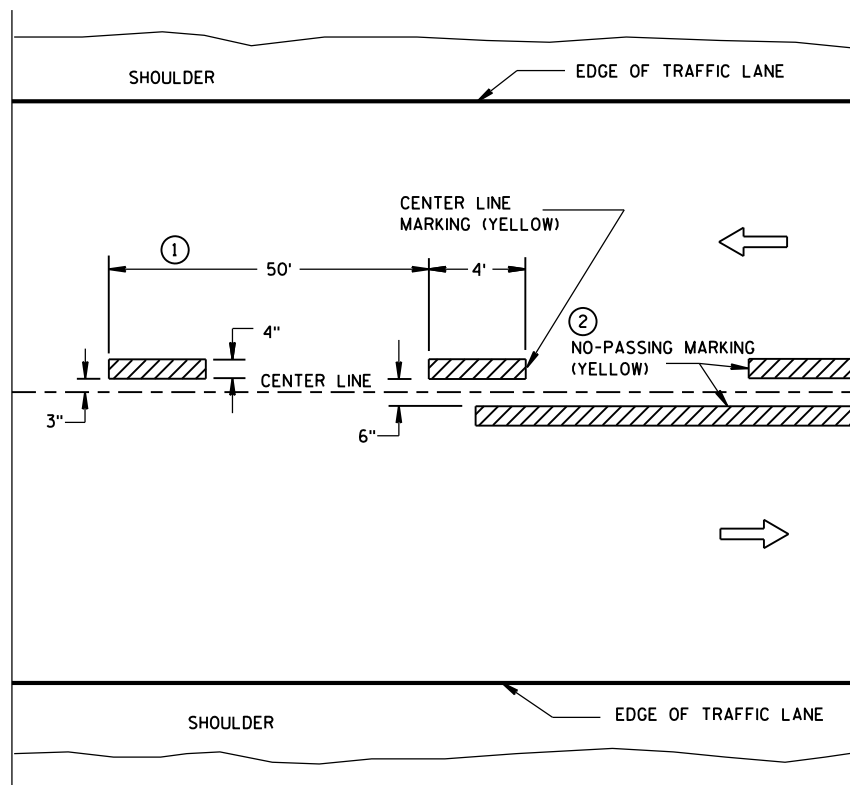


TWO WAY TRAFFIC

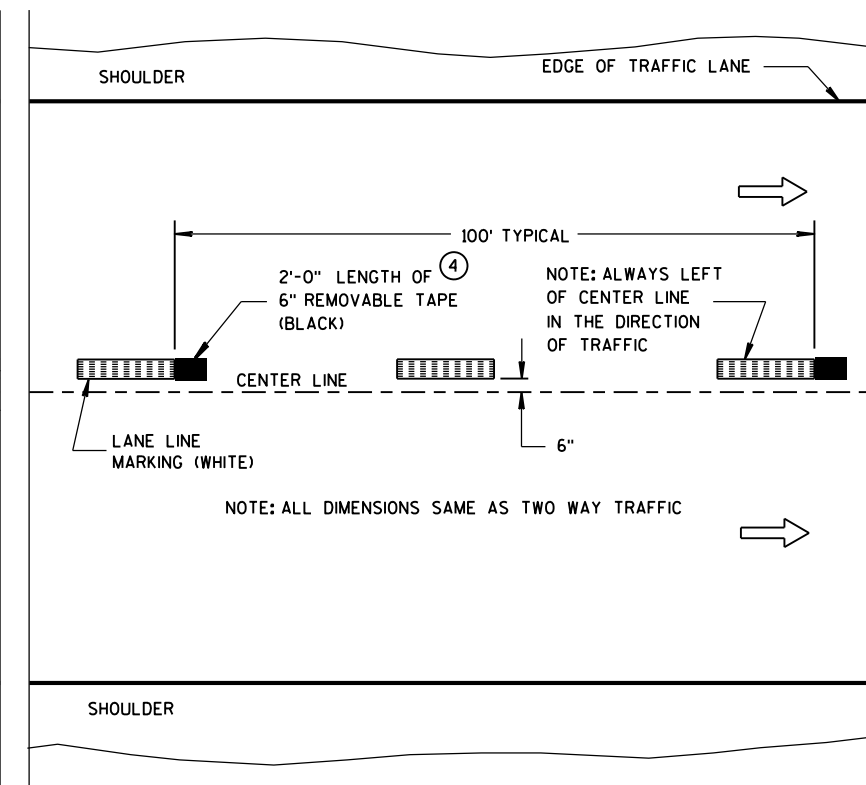


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

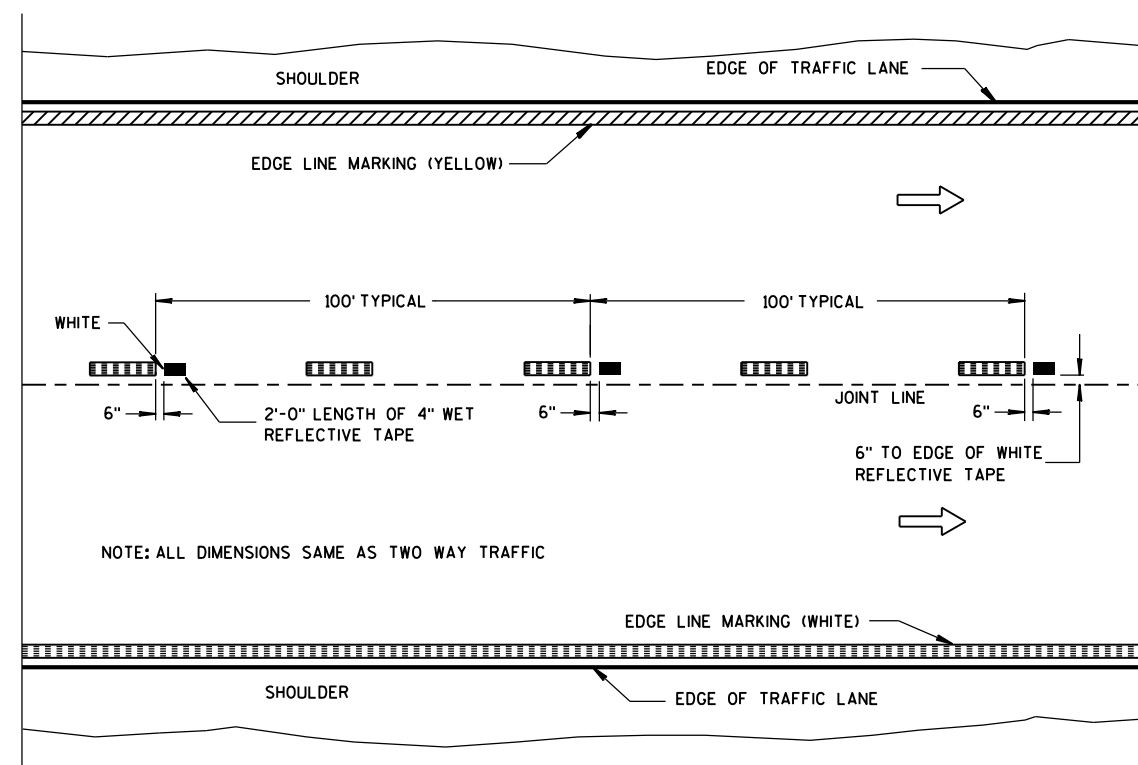
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- ② NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- ③ NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- ④ CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

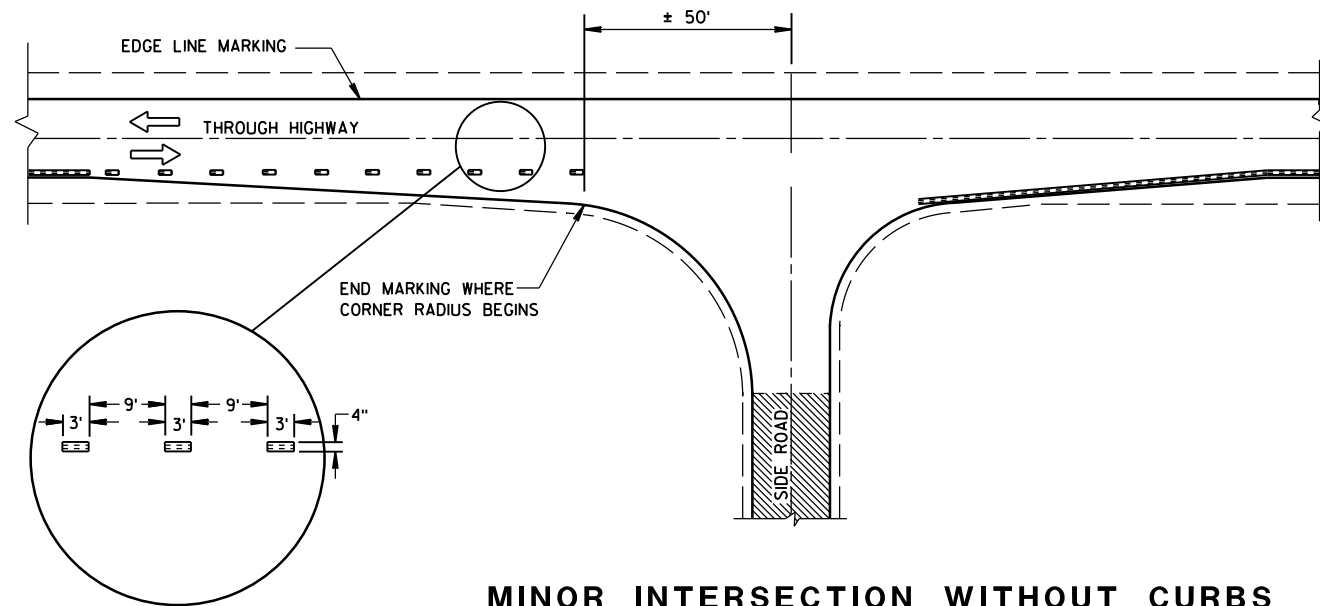
- "T" MARKING
- POST MOUNTED SIGN

PAVEMENT MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013
DATE
FHWA

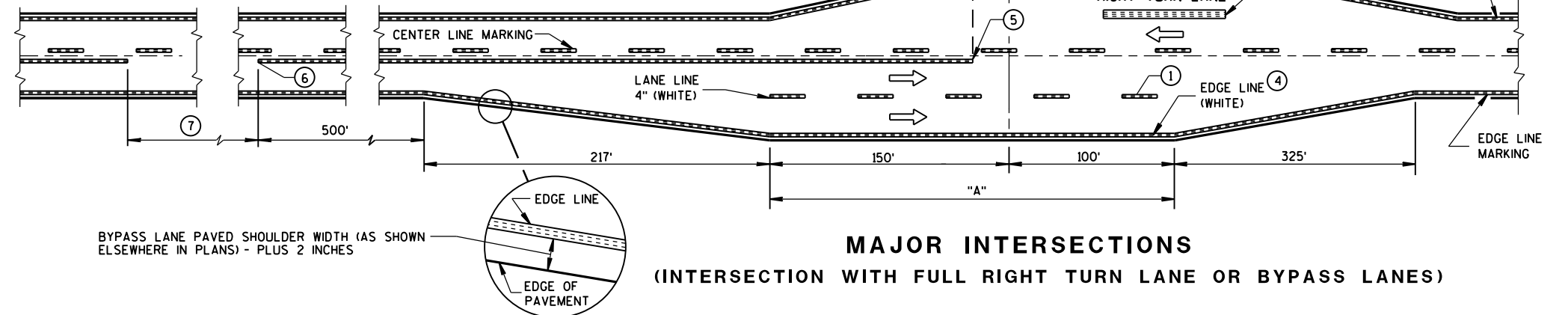
/S/ Travis Feltes
STATE TRAFFIC ENGINEER



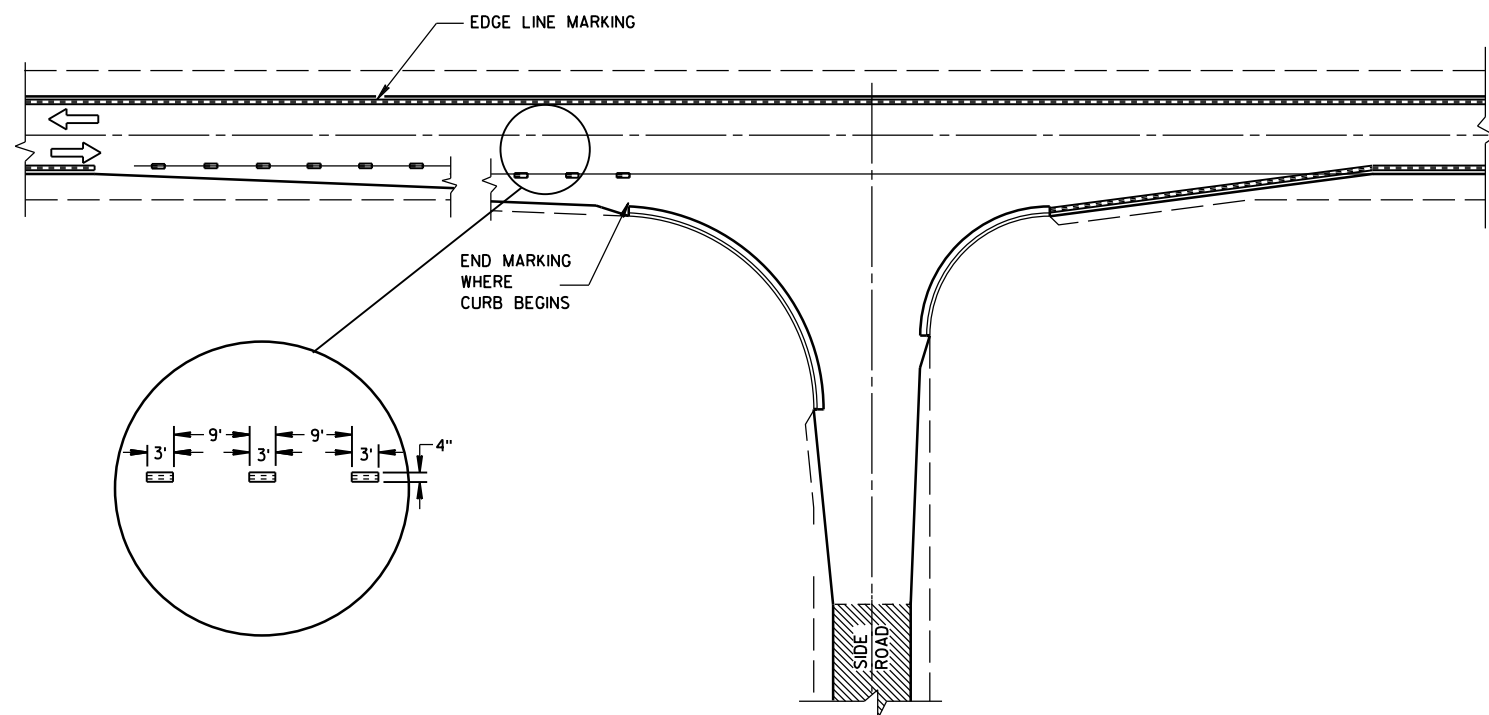
MINOR INTERSECTION WITHOUT CURBS

⑦

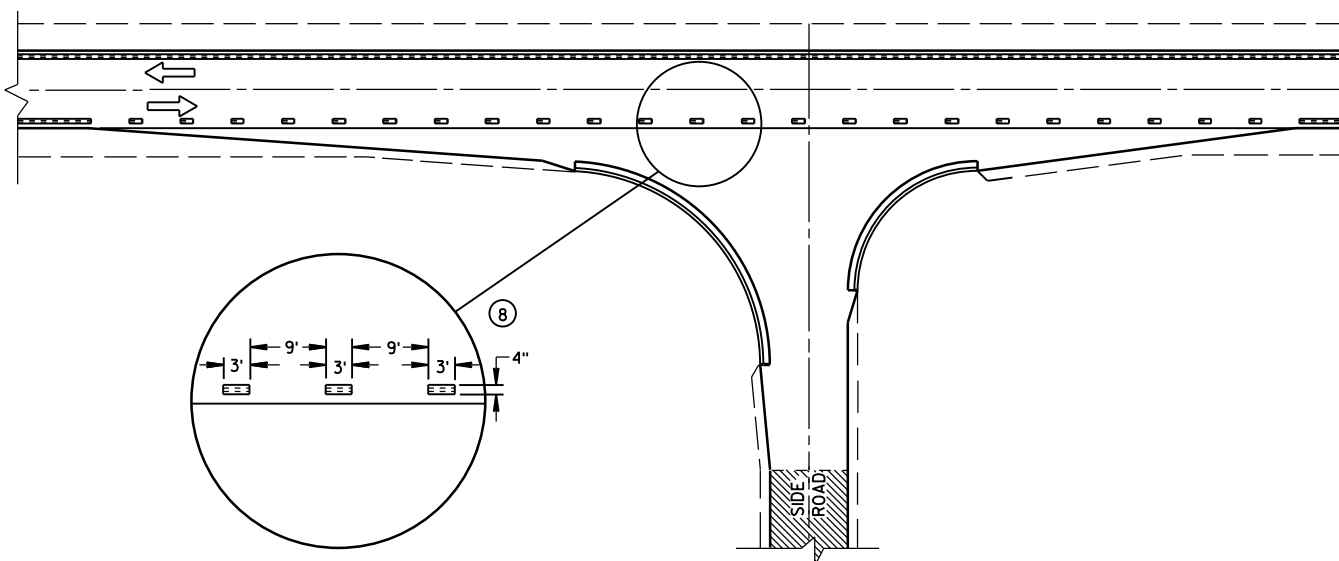
POSTED SPEED (MPH)	MINIMUM DISTANCE BETWEEN ZONES (FEET)
25 - 30	528
35 - 40	528
45 - 50	686
55	792



MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANES)



MINOR INTERSECTION WITH CURBS
(TYPICAL MARKING)



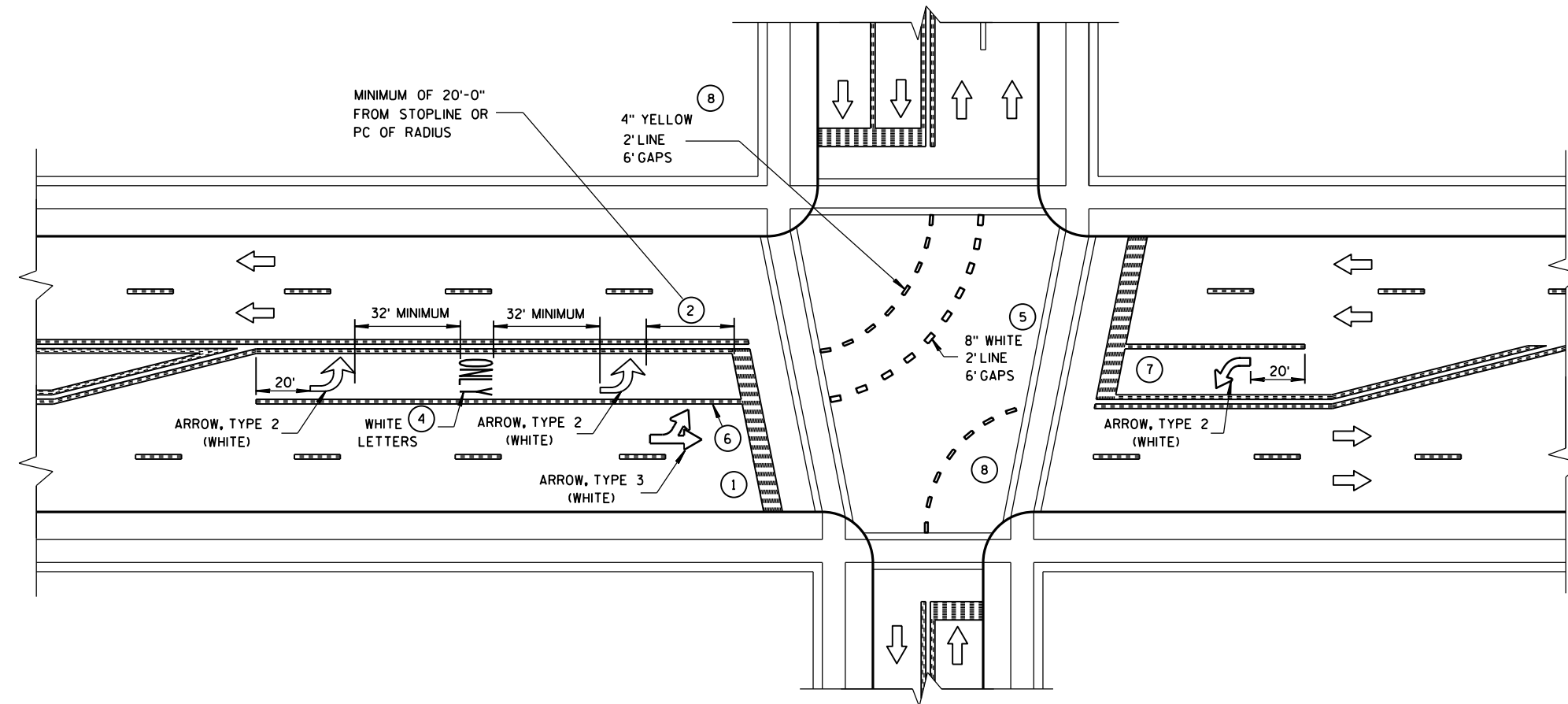
MINOR INTERSECTION WITH CURBS
③ (FOR SPECIAL CONDITIONS AS SPECIFIED)

GENERAL NOTES

- EDGE LINES SHALL BE OMITTED THROUGH INTERSECTIONS. EDGE LINES SHALL BE CONTINUED THROUGH DRIVEWAYS.
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
 - ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
 - ③ ALTERNATIVE MARKING SHALL BE PROVIDED WHEN SPECIFIED IN THE CONTRACT. TYPICAL SITUATIONS WHERE THIS MARKING MAY BE REQUIRED ARE WHERE THE INTERSECTION IS ON A SHARP HORIZONTAL CURVE OR CREST VERTICAL CURVE IN AN UNLIGHTED AREA SUCH THAT THE EDGE LINE MAY BE MISLEADING TO THE MOTORIST OR DISAPPEAR FROM SIGHT.
 - ④ THE EDGE LINE IN THE TAPER AREAS OF THE BYPASS LANE AND THE BYPASS LANE SHALL BE LOCATED 1-FOOT FROM EDGE OF PAVEMENT TO THE OUTSIDE EDGE OF EDGE LINE.
 - ⑤ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT/SURFACE EDGE EXTENSION.
 - ⑥ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
 - ⑦ IF THE DISTANCE BETWEEN 2 SUCCESSIVE NO-PASSING ZONES IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES, CONNECT THE 2 ZONES.
 - ⑧ 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE - GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL

PAVEMENT MARKING
(INTERSECTIONS)

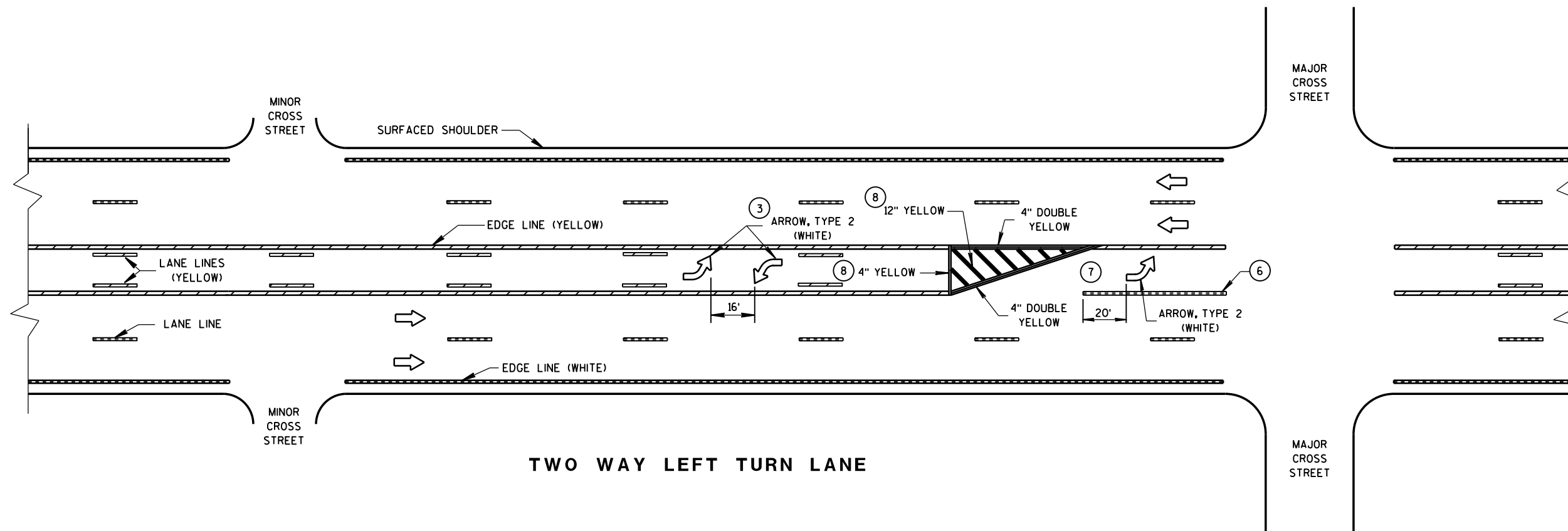
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



GENERAL NOTES

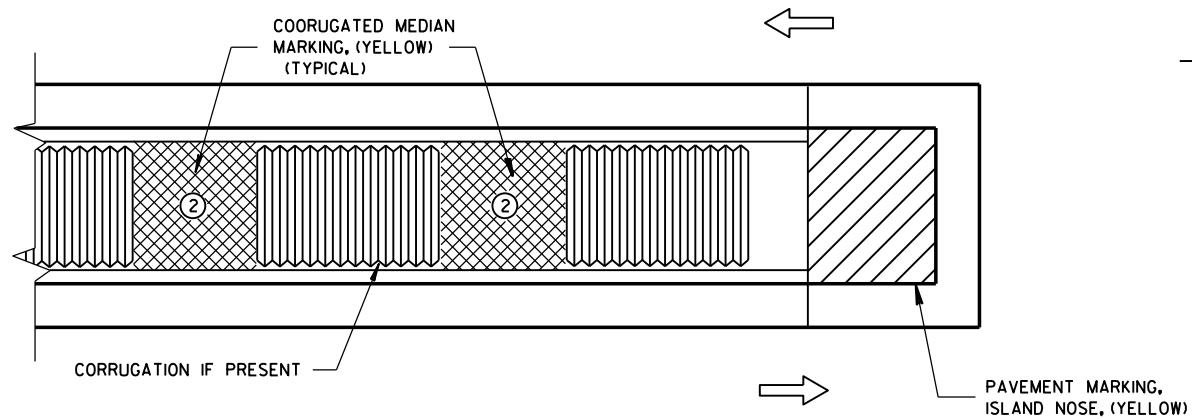
- ① STOP BAR IS REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.
- ② DISTANCE MAY BE ADJUSTED TO ACCOMMODATE SHORT LEFT TURN LANES, AS APPROVED BY THE ENGINEER.
- ③ A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ④ ADD EXTRA SETS OF ONE ARROW AND ONE ONLY PER 160 FEET OR WHEN ON A CURVE.
- ⑤ 8" WHITE WITH 2' LINE 6' GAPS FOR DUAL TURN LANE.
- ⑥ 8" WHITE
- ⑦ ADD SECOND ARROW WHEN TURN BAY IS GREATER THAN OR EQUAL TO 108 FEET.
- ⑧ REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT.

NOTE:
ARROW SYMBOL (➡)
SHOWS DIRECTION OF TRAVEL

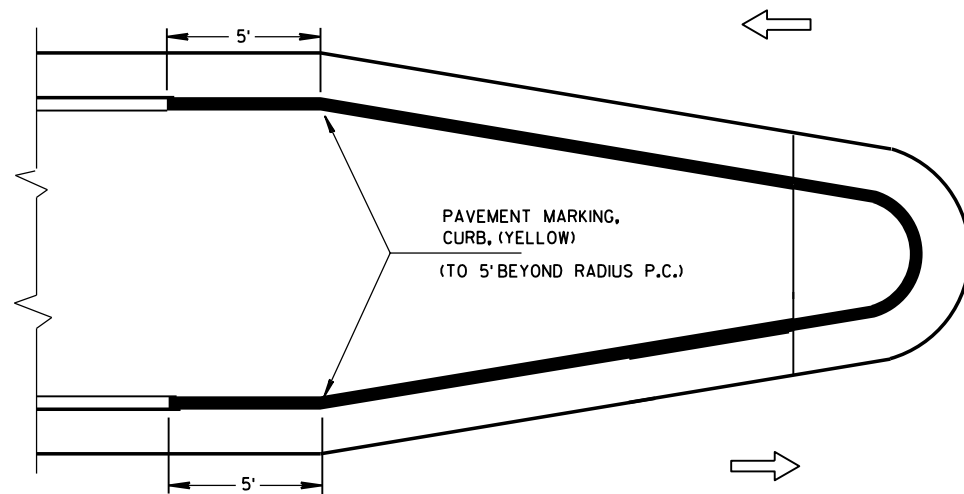


PAVEMENT MARKING
(LEFT TURN LANE)

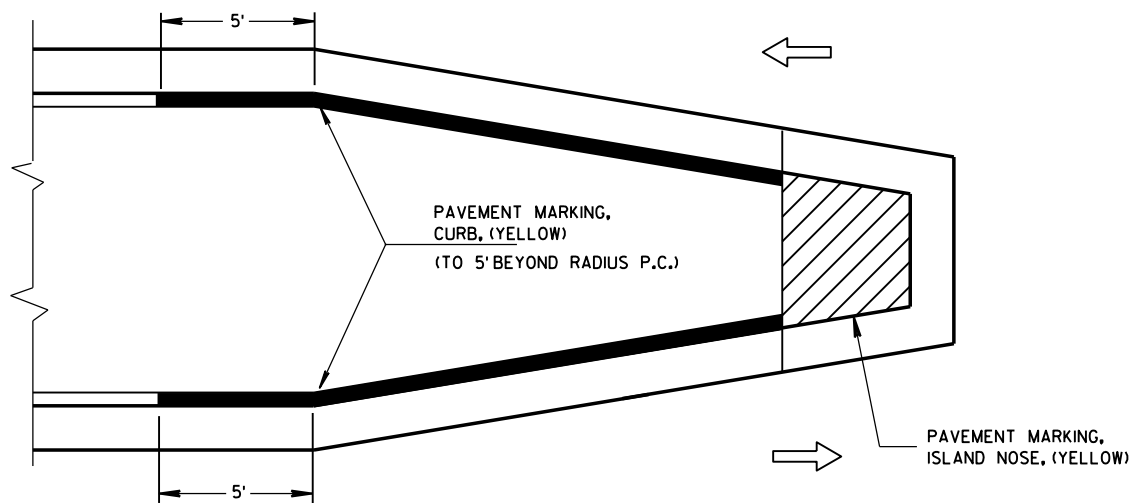
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MEDIAN ISLAND WITH SQUARE BLUNT NOSE

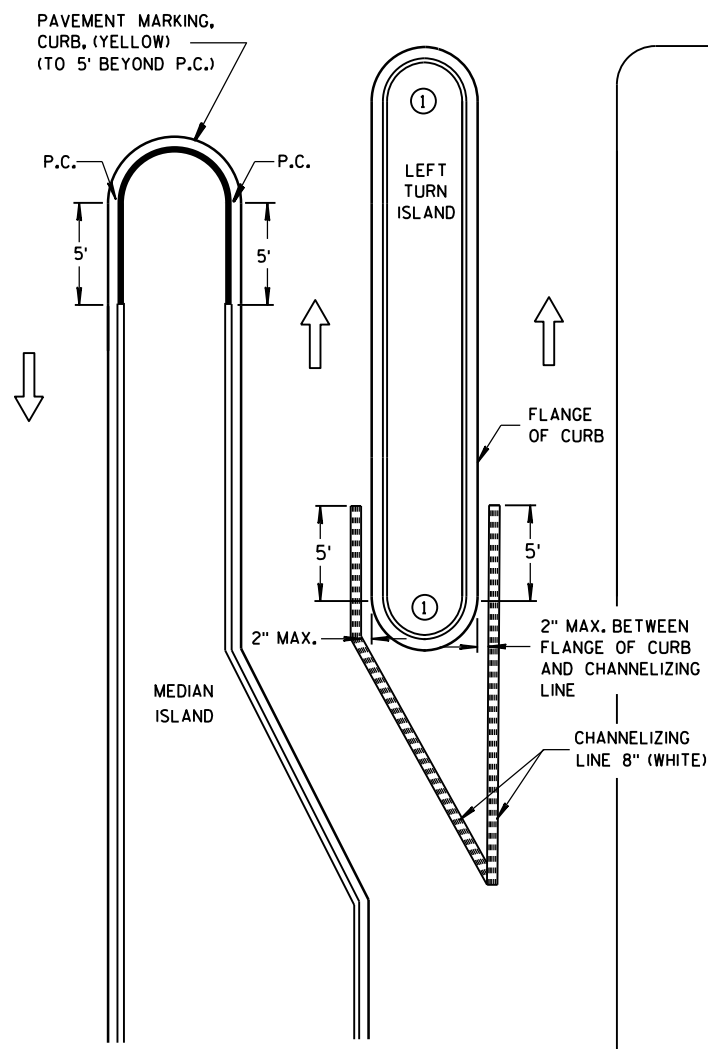


MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

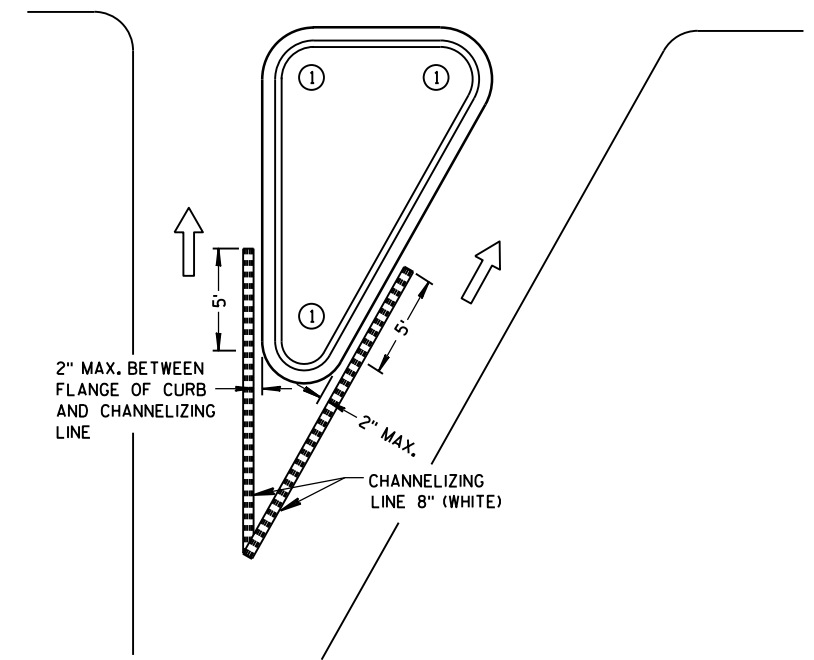
TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS



LEFT TURN & MEDIAN ISLAND

GENERAL NOTES

- 1 DO NOT MARK CURB NOSES THAT SEPARATE LANES OF TRAFFIC TRAVELING IN THE SAME DIRECTION.
- 2 WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.



RIGHT TURN ISLAND





LEGEND

- ISLAND NOSE MARKING
- CURB MARKING
- CORRUGATED MEDIAN MARKING
- DIRECTION OF TRAVEL

PAVEMENT MARKING (ISLANDS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

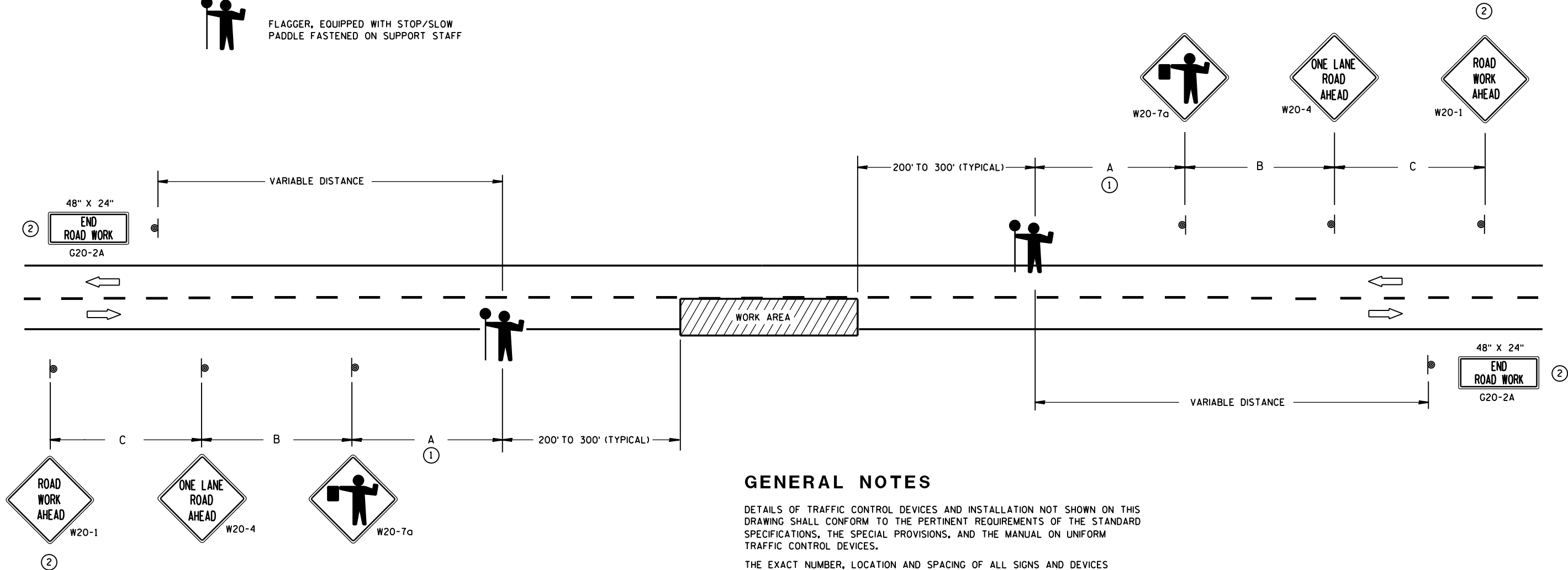
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

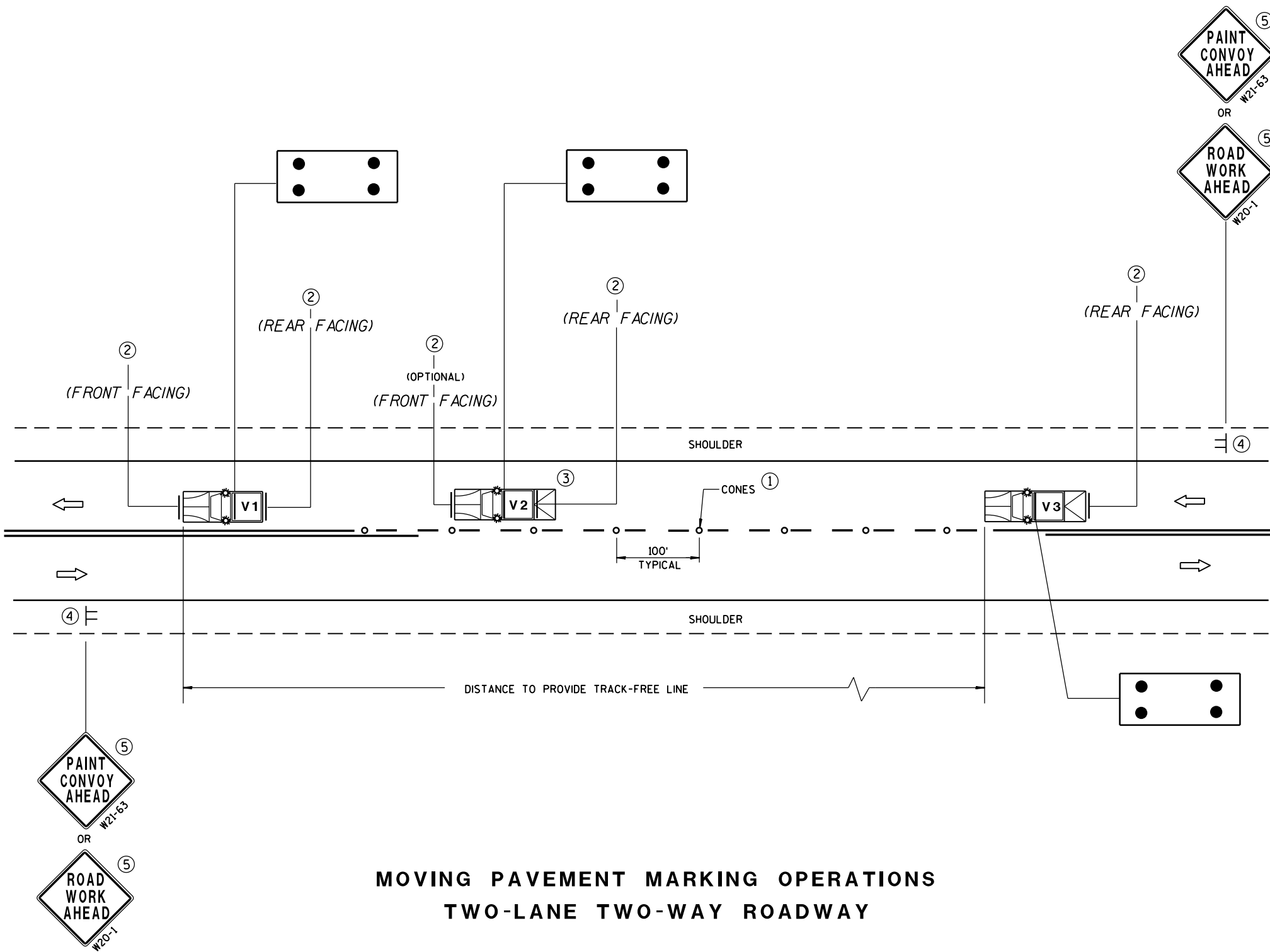
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

TRAFFIC CONTROL FOR LANE
CLOSURE (SUITABLE FOR
MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGELINE MARKING.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

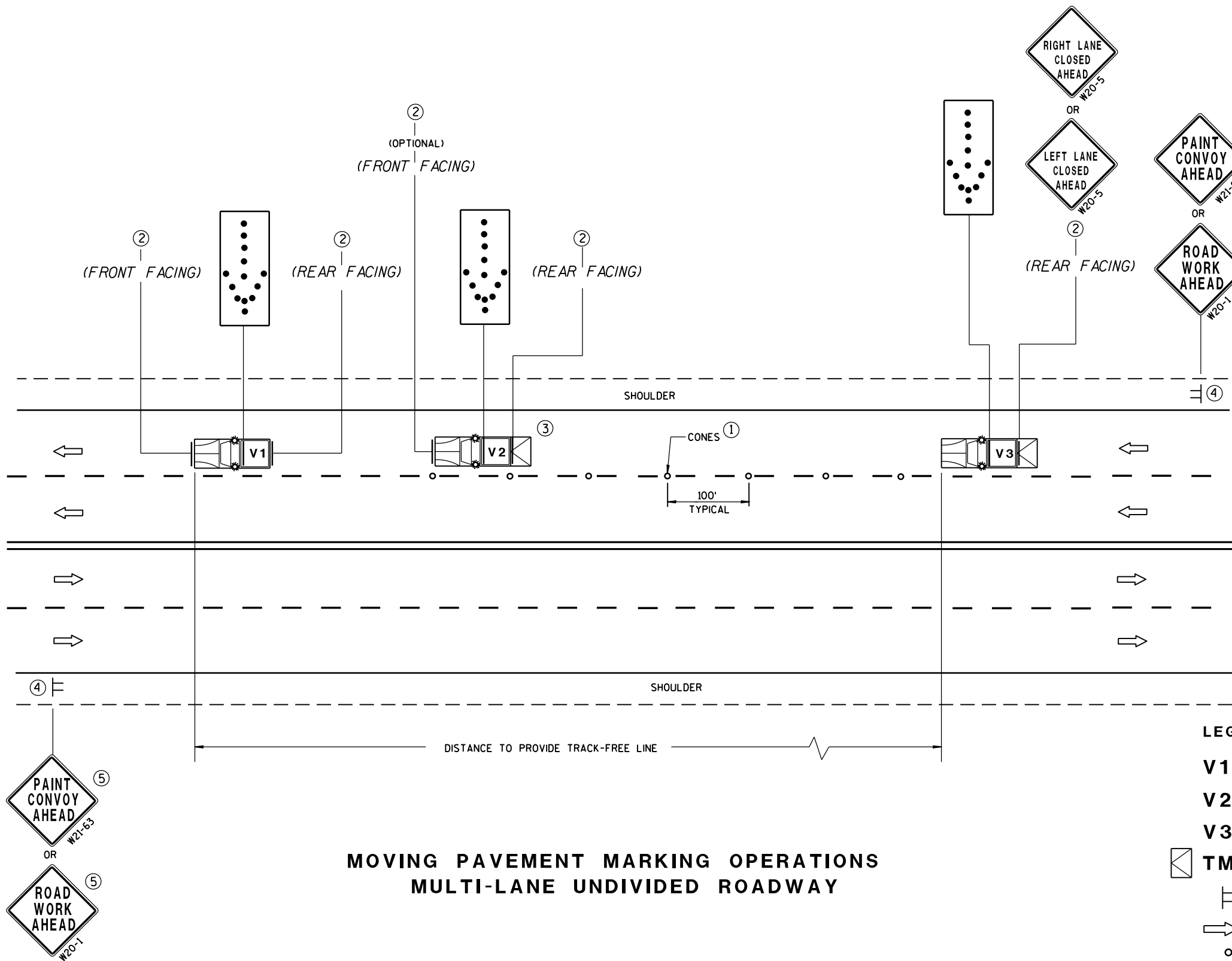
LEGEND

- V1 LEAD VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE WITH TMA
- TMA TRUCK-MOUNTED ATTENUATOR
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- CONES
- FLASHING ARROW PANEL (CAUTION)

MOVING PAVEMENT MARKING
OPERATION
TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER
FHWA

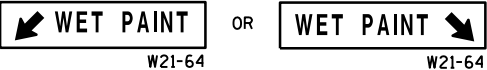


MOVING PAVEMENT MARKING OPERATIONS
MULTI-LANE UNDIVIDED ROADWAY

GENERAL NOTES

- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.
- IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.
- ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.
- DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.
- WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.
- USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.
- FOR EDGE LINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH.
- WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.
- THIS DRAWING SHALL BE USED FOR CENTERLINE OR EDGE LINE OR LANELINE MARKING FOR MULTILANE UNDIVIDED ROADWAYS.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.



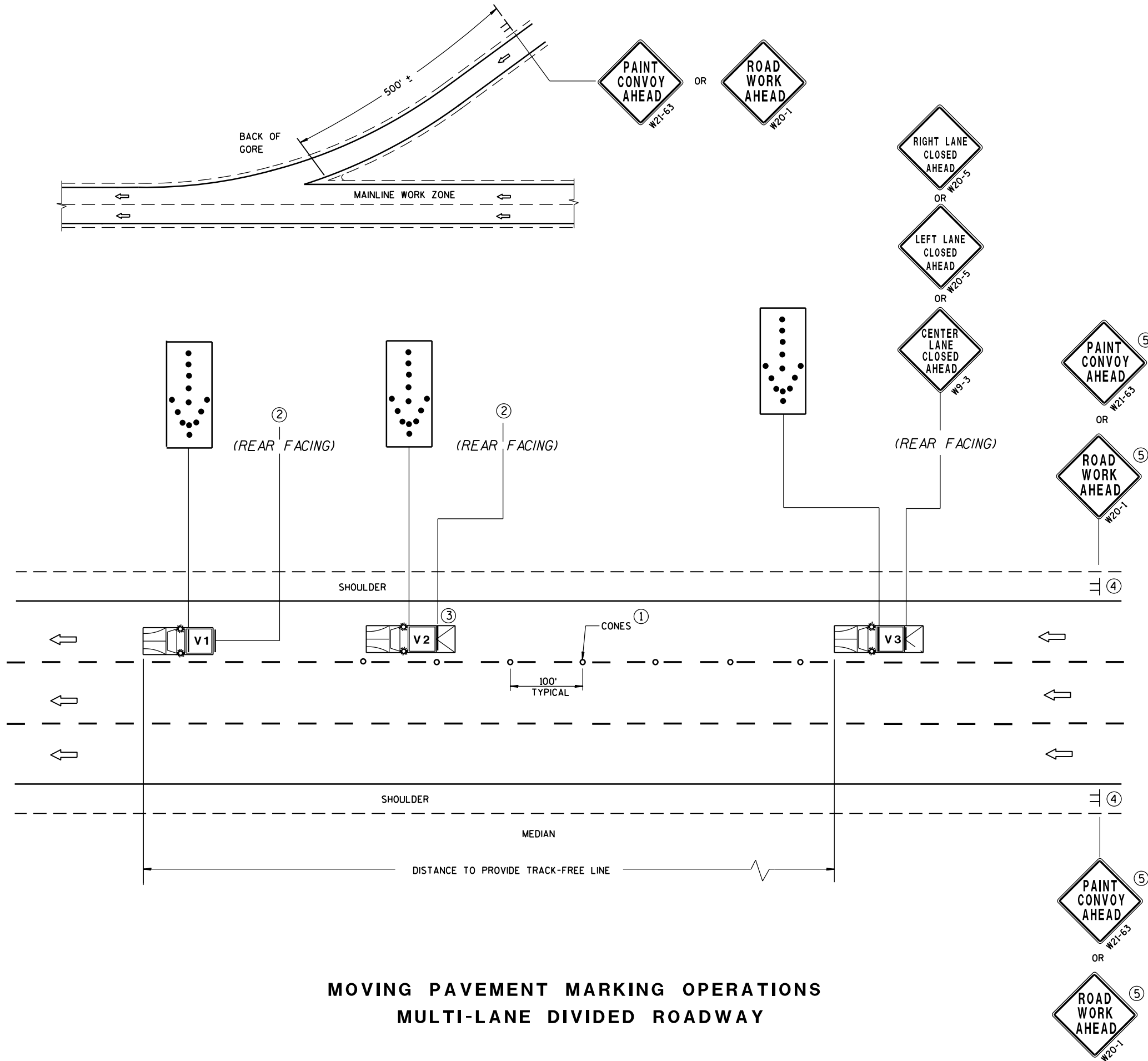
LEGEND

- V1 LEAD VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE WITH TMA
- TMA TRUCK-MOUNTED ATTENUATOR
- Sign on Temporary Support
- Direction of Traffic
- Cones
- Flashing Arrow Panel (Merge)

MOVING PAVEMENT MARKING
OPERATION
MULTI-LANE UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER
FHWA



GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

IF SPEED LIMIT IS 40 MPH OR LESS STATIONARY SIGNS MAY BE OMITTED IF CONES ARE USED.

ALTERNATE SIGN MESSAGES, SUCH AS "PAINT CREW AHEAD" OR "ROAD PAINTING AHEAD" MAY BE USED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

FOR EDGE LINE MARKING OR IF CONES ARE NOT USED, POSITION THE REARMOST SHADOW VEHICLE ON THE SHOULDER AS SHOWN IN THE MUTCD IF THE SHOULDER HAS ADEQUATE WIDTH. USE DOUBLE ARROWS WHEN CONVOY IS IN CENTER LANE ONLY.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR TURN THE STATIONARY WARNING SIGNS AWAY FROM TRAFFIC.

THIS DRAWING SHALL BE USED FOR EDGE LINE OR LANELINE MARKING FOR MULTILANE DIVIDED ROADWAYS.

- ① CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.
- ② USE STANDARD SIGN W21-64 WITH APPROPRIATE ARROW.
- ③ OPTIONAL TRUCK-MOUNTED ATTENUATOR.
- ④ SIGNS SHALL BE REPEATED AFTER EVERY ON RAMP OR EVERY THREE MILES.
- ⑤ IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1 OR W21-63 ARE NOT REQUIRED.

LEGEND

- V1 LEAD VEHICLE
- V2 SHADOW VEHICLE
- V3 TRAIL VEHICLE WITH TMA
- TMA TRUCK-MOUNTED ATTENUATOR
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- CONES
- FLASHING ARROW PANEL (MERGE)

MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 5/3/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER
FHWA	

GENERAL NOTES

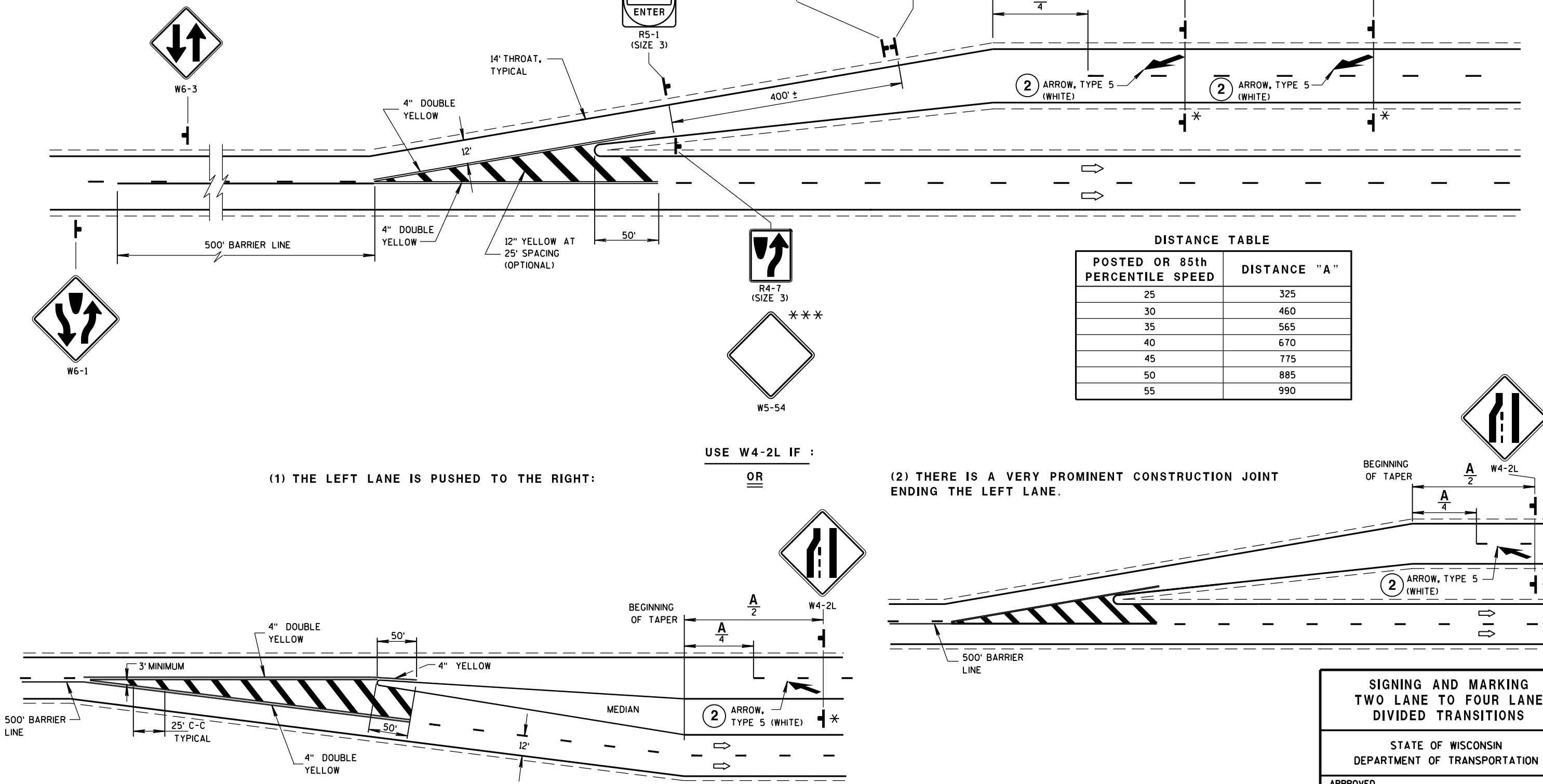
SIGNING AND MARKING IS SHOWN AS TYPICAL PLACEMENT. FIELD CONDITIONS MAY DICTATE CHANGES IN SIGNING AND MARKING PLACEMENT.

A DISTANCE DEPENDENT ON SPEED (SEE TABLE)

- 1 WRONG WAY PREVENTION SIGNS ARE DISCUSSED IN T G M 2-15-12
- 2 REQUIRED ONLY WHEN SPECIFIED IN THE CONTRACT

SYMBOLS

- * OPTIONAL SIGNS
- ** SIGNS MAY BE OMITTED IF SPACE DOES NOT PERMIT
- *** IF POSTED SPEED 45 MPH OR GREATER, PLACE W5-54 SIGN UNDER R4-7 SIGN. MOUNT W5-54 SIGN AT 4' MOUNTING HEIGHT (TOP OF ROADWAY TO BOTTOM OF SIGN)
- POST MOUNTED SIGN
- DIRECTION OF TRAFFIC FLOW



DISTANCE TABLE

POSTED OR 85th PERCENTILE SPEED	DISTANCE "A"
25	325
30	460
35	565
40	670
45	775
50	885
55	990

(1) THE LEFT LANE IS PUSHED TO THE RIGHT:

USE W4-2L IF :
OR

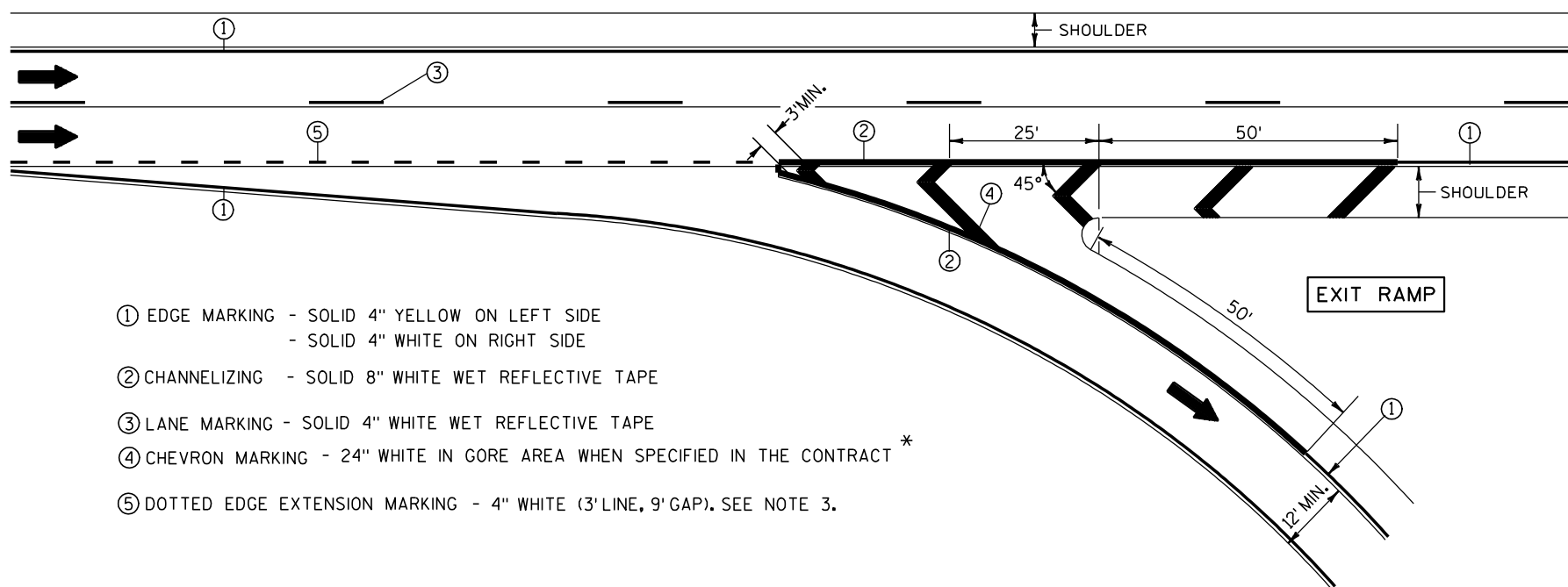
(2) THERE IS A VERY PROMINENT CONSTRUCTION JOINT ENDING THE LEFT LANE.

SIGNING AND MARKING
TWO LANE TO FOUR LANE
DIVIDED TRANSITIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5/3/2013 DATE /S/ Travis Feltes
STATE TRAFFIC ENGINEER

FHWA

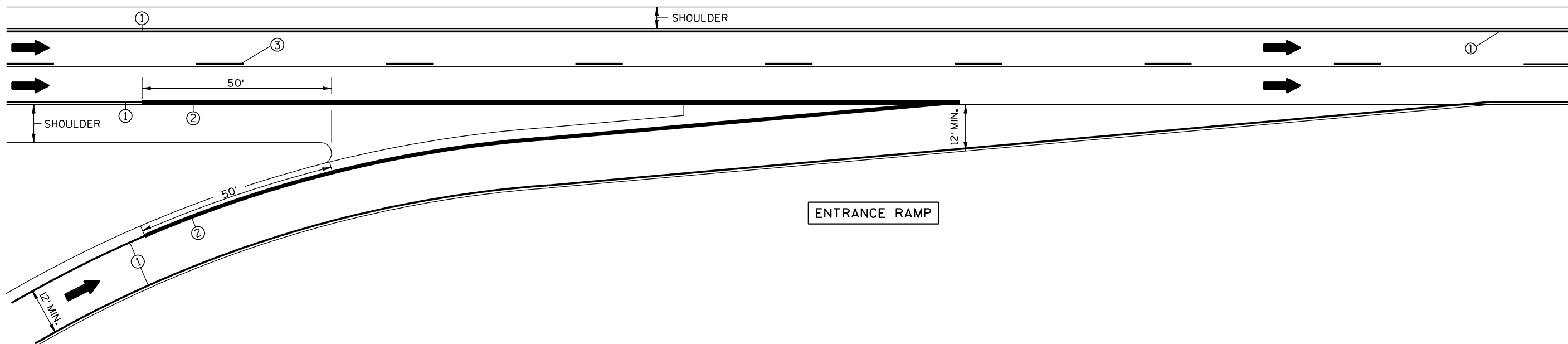


- ① EDGE MARKING - SOLID 4" YELLOW ON LEFT SIDE
- SOLID 4" WHITE ON RIGHT SIDE
- ② CHANNELIZING - SOLID 8" WHITE WET REFLECTIVE TAPE
- ③ LANE MARKING - SOLID 4" WHITE WET REFLECTIVE TAPE
- ④ CHEVRON MARKING - 24" WHITE IN GORE AREA WHEN SPECIFIED IN THE CONTRACT *
- ⑤ DOTTED EDGE EXTENSION MARKING - 4" WHITE (3' LINE, 9' GAP). SEE NOTE 3.

NOTES:

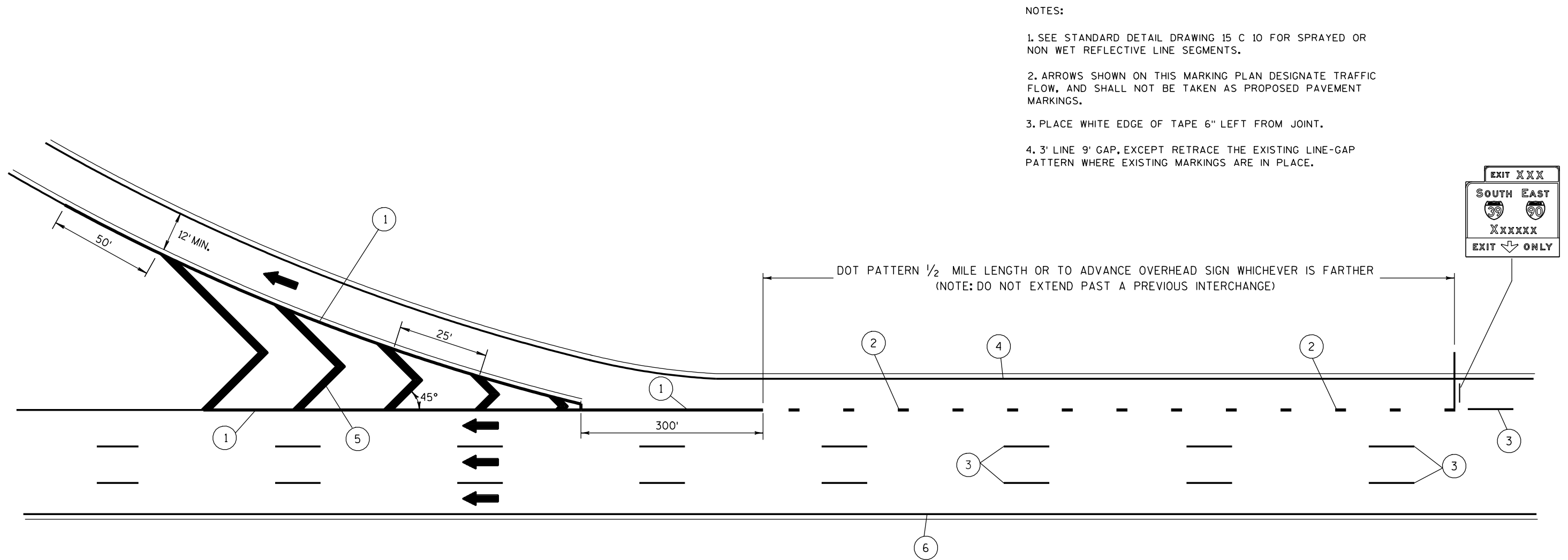
- 1. ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
- 2. PLACE WHITE EDGE OF TAPE 6" LEFT FROM JOINT.
- 3. 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- 4. RETRACE EXISTING DIAGONAL MARKINGS.

* REFER TO DESIGN NOTES.



PAVEMENT MARKING
(RAMPS AND GORES)

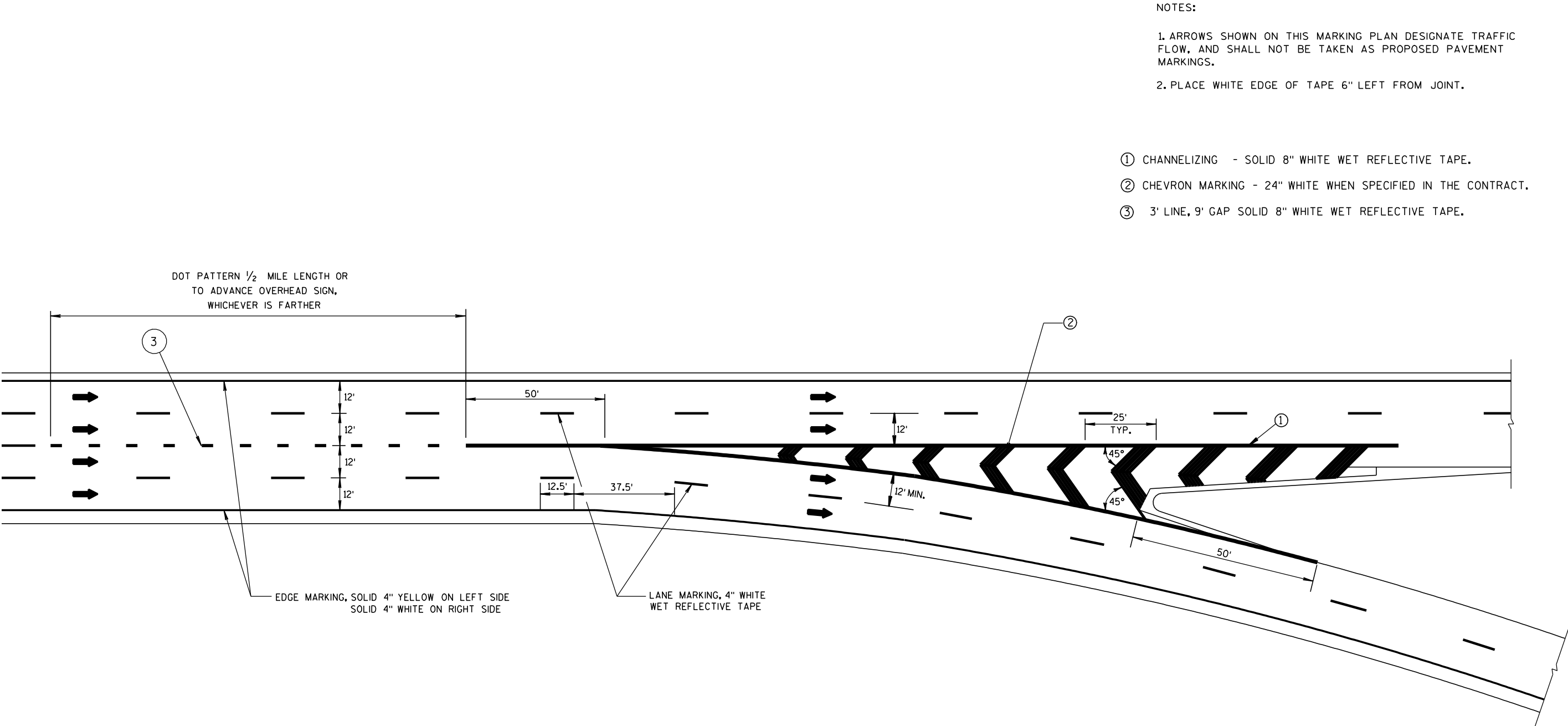
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



- ① CHANNELIZING - SOLID 8" WHITE WET RELECTIVE TAPE
- ② 3' LINE, 9' GAP SOLID 8" WHITE WET REFLECTIVE TAPE. SEE NOTE 4.
- ③ SOLID 4" WHITE WET REFLECTIVE TAPE
- ④ 4" WHITE EDGELINE
- ⑤ CHEVRON MARKING - 24" WHITE WHEN SPECIFIED IN THE CONTRACT
- ⑥ 4" YELLOW EDGELINE

LANE DROP
PAVEMENT MARKING

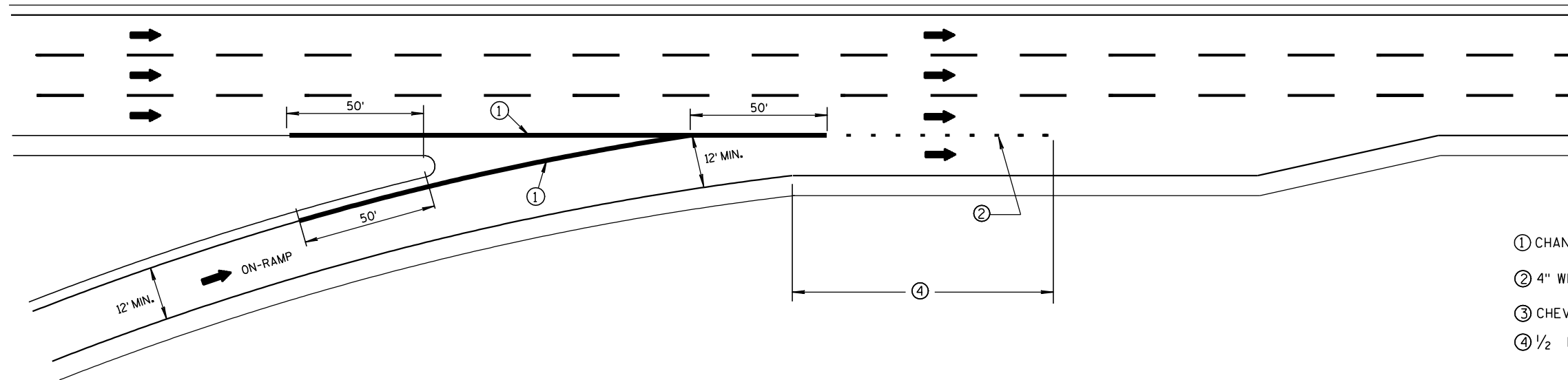
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



- NOTES:
1. ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
2. PLACE WHITE EDGE OF TAPE 6" LEFT FROM JOINT.
- ① CHANNELIZING - SOLID 8" WHITE WET REFLECTIVE TAPE.
- ② CHEVRON MARKING - 24" WHITE WHEN SPECIFIED IN THE CONTRACT.
- ③ 3' LINE, 9' GAP SOLID 8" WHITE WET REFLECTIVE TAPE.

PAVEMENT MARKING
MAJOR SPLIT
FREEWAY TO FREEWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

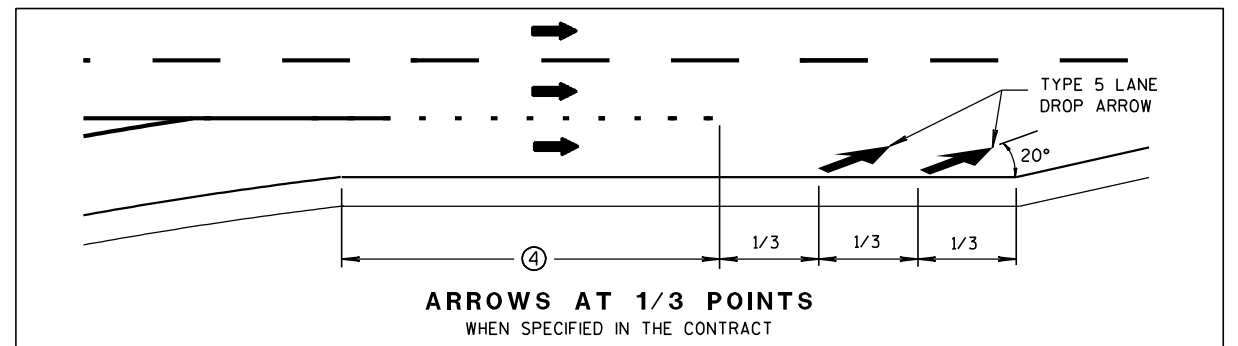
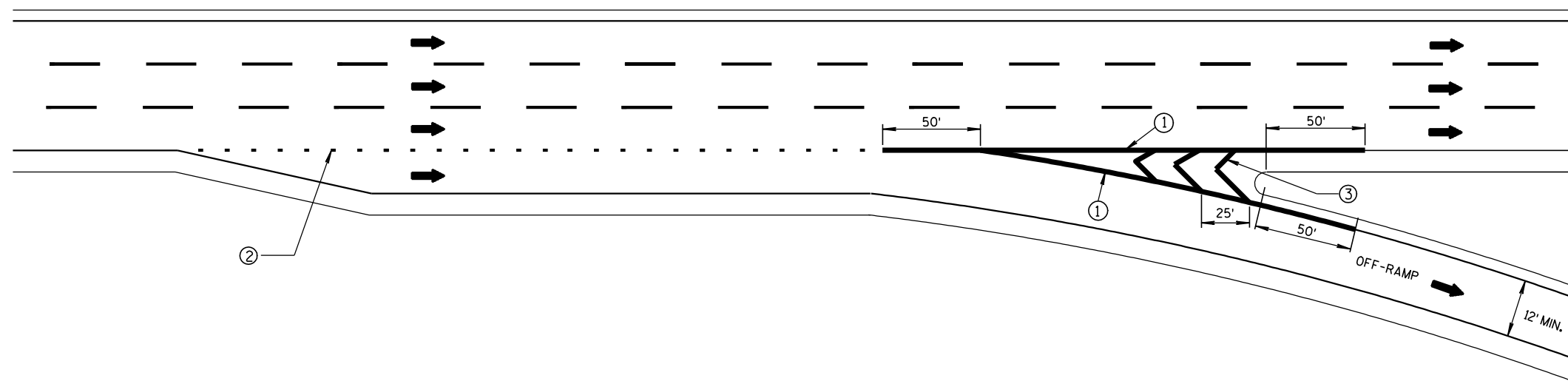


SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL ENTRANCE-RAMP

NOTES:

1. AHEAD ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
2. PLACE WHITE EDGE OF TAPE 6" LEFT FROM JOINT.
3. RETRACE EXISTING DIAGONAL MARKINGS.

- ① CHANNELIZING - SOLID 8" WHITE WET REFLECTIVE TAPE IN GORE AREA.
- ② 4" WHITE (3' LINE, 9' GAP).
- ③ CHEVRON MARKING - 24" WHITE WHEN SPECIFIED IN THE CONTRACT.
- ④ ½ LENGTH OF FULL WIDTH ACCELERATION LANE.

ARROWS AT 1/3 POINTS
WHEN SPECIFIED IN THE CONTRACT

SERVICE INTERCHANGE PAVEMENT MARKING FOR PARALLEL EXIT-RAMP

PAVEMENT MARKING FOR
PARALLEL ON-RAMP AND
PARALLEL OFF-RAMPSTATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

6/23/2011
DATE

FHWA

/S/ Thomas N. Notbohm
STATE TRAFFIC ENGINEER OF DESIGN

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMENENT SUPPORT
- POST WITH ATTACHED SIGN IN DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- WORK AREA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

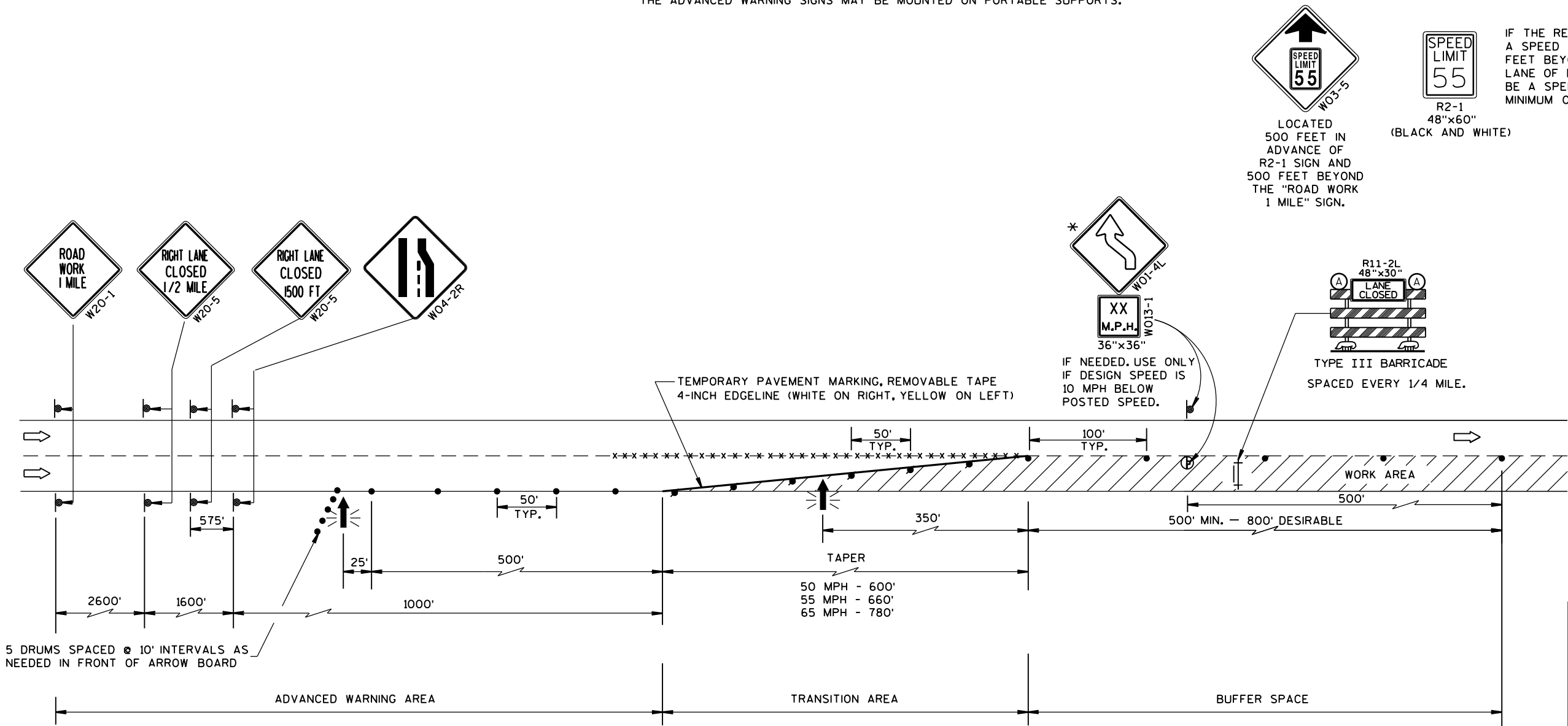
REMOVE PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR LONGER THAN 4 OR MORE DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.







ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE CLOSURE MUST MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

* THE LEFT REVERSE CURVE SIGN (WO1-4L) IS ONLY REQUIRED WHEN THIS DETAIL IS USED IN COMBINATION WITH "SINGLE LANE CROSSOVER" DETAIL.



TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M.P.H.	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	

LEGEND

-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TRAFFIC CONTROL DRUM
-  SIGN ON PERMANENT SUPPORT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

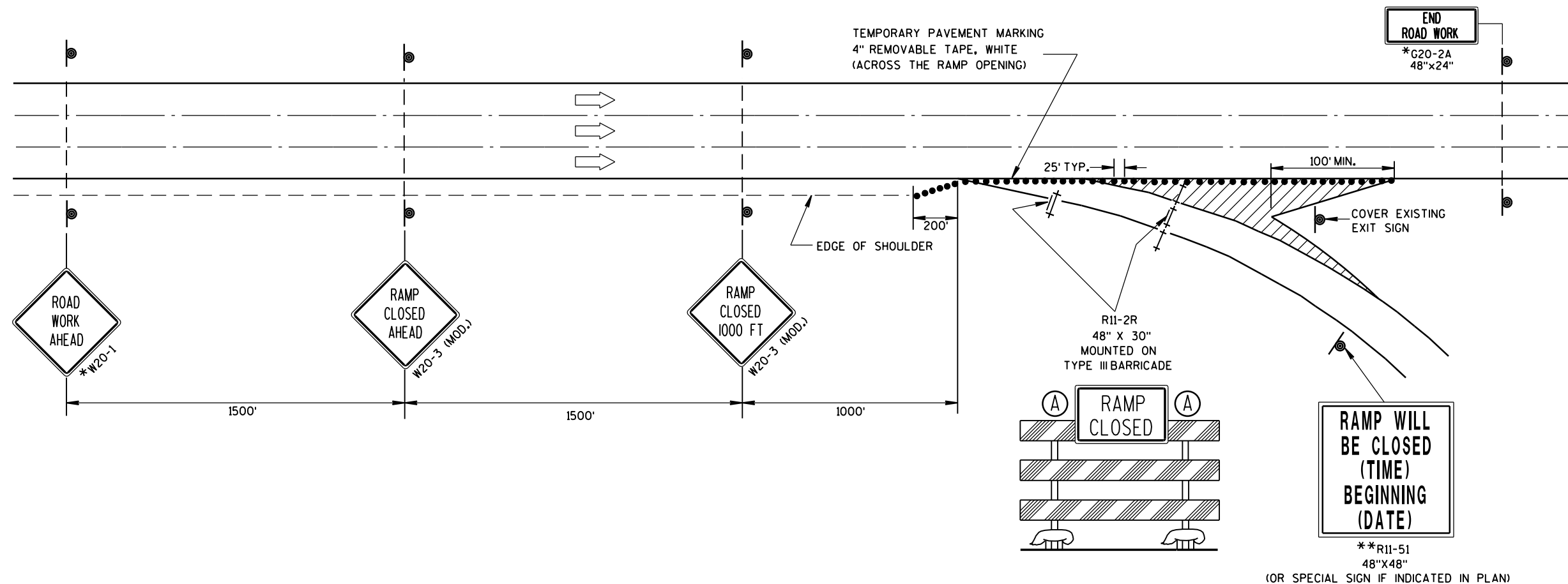
PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF RAMP CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROPOFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN THE WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12-HOUR DURATION.

*W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

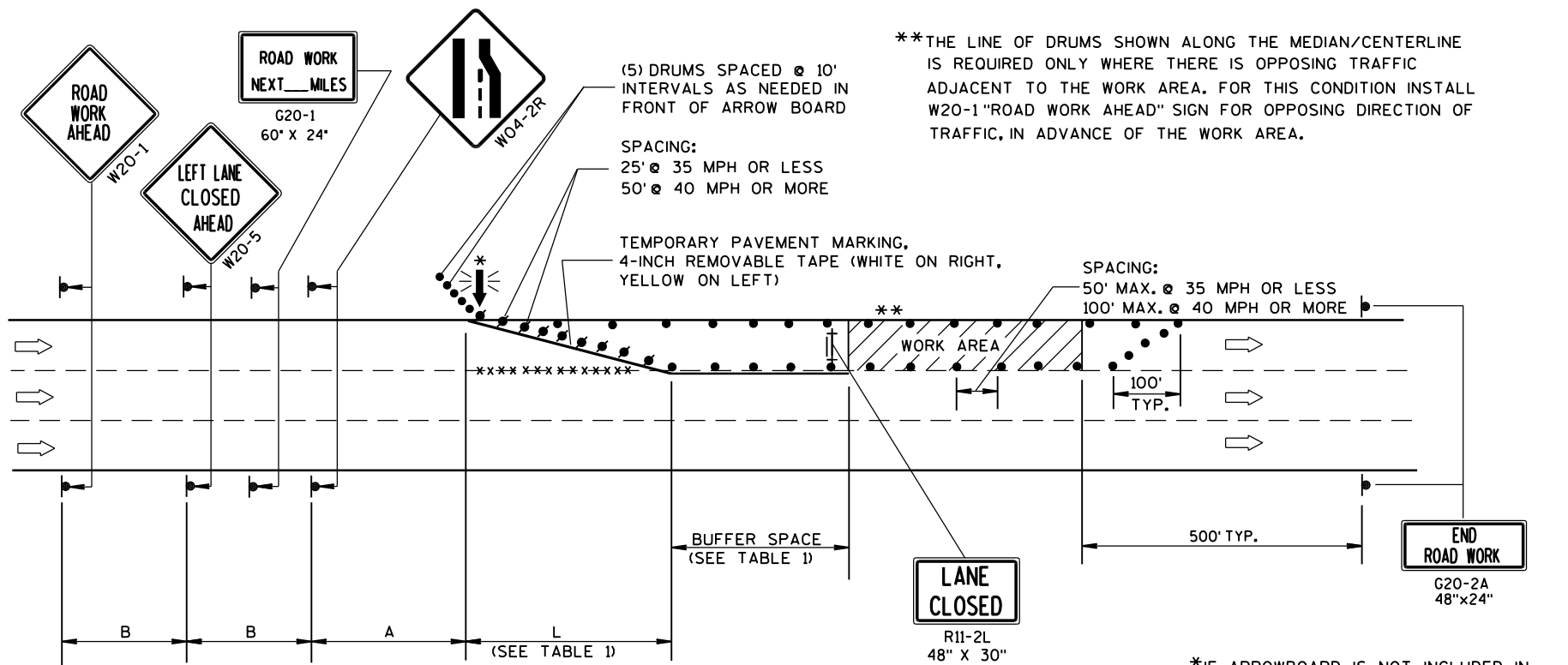
** PLACE "RAMP WILL BE CLOSED" SIGN 10 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



TRAFFIC CONTROL,
EXIT RAMP CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



B=400' AT 25-30 MPH
700' AT 35-40 MPH
1000' AT 45-55 MPH

A=200' AT 25-30 MPH
350' AT 35-40 MPH
500' AT 45-55 MPH

TABLE 1
TAPER AND BUFFER SPACE
FOR 12' LANE WIDTH

S	L	BUFFER SPACE
25	125'	55'
30	180'	85'
35	245'	120'
40	320'	170'
45	540'	220'
50	600'	280'
55	660'	335'

FOR LANE WIDTH OTHER THAN 12':

L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

L = TAPER LENGTH IN FEET

S = NON-CONSTRUCTION SPEED LIMIT (MPH)

W = WIDTH OF LANE CLOSURE

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

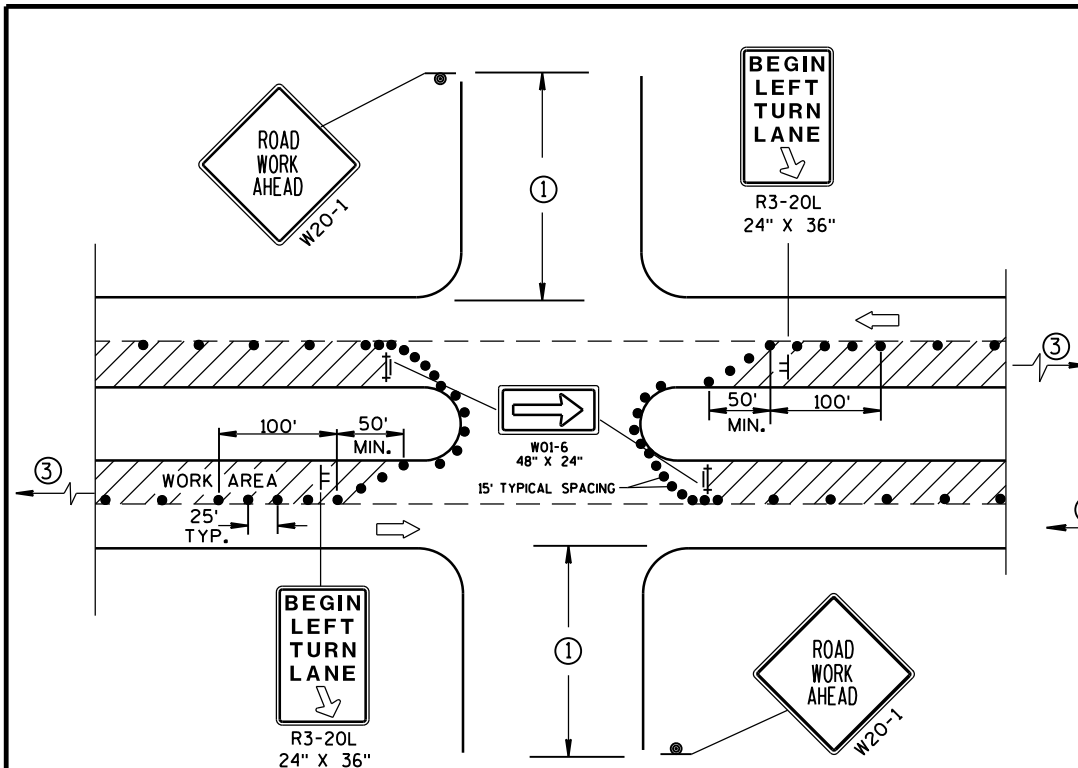
GENERAL NOTES

- THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.
- THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.
- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.
- ON UNDIVIDED ROADWAYS, OMIT THE SIGNS SHOWN ON LEFT SIDE OF ROAD.
- W20-1, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.
- OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.
- CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.
- PLACE THE ARROWBOARD AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE LANE CLOSURE TAPER, PREFERABLY ON THE SHOULDER OR TERRACE.
- CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.
- BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

TRAFFIC CONTROL,
SINGLE LANE CLOSURE,
NON-FREEWAY/EXPRESSWAY

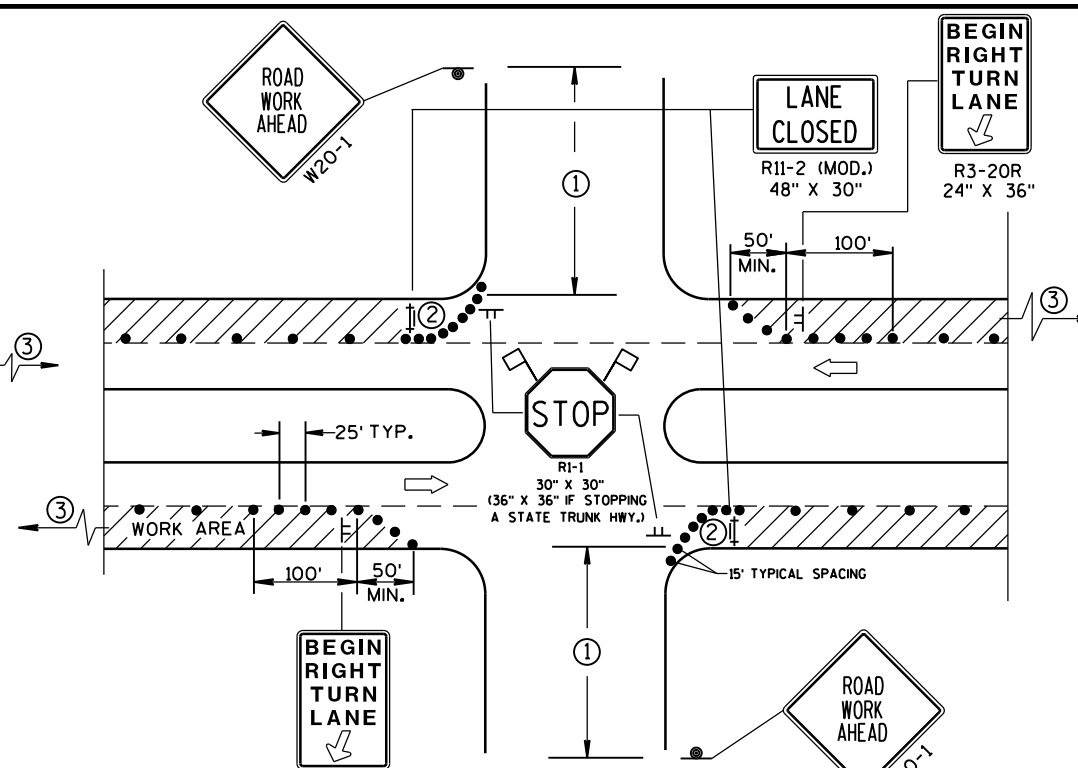
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA



DETAIL A
FOR LEFT LANE CLOSURE AT
INTERSECTION OR MEDIAN OPENING

PROVIDE TURN LANES AT
INTERSECTIONS WHENEVER
STAGING OF WORK ALLOWS.
TAPER AND TURN LANE
LENGTHS BASED ON FIELD
CONDITIONS AS APPROVED
BY THE ENGINEER.



DETAIL B
FOR RIGHT LANE CLOSURE
AT INTERSECTION

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

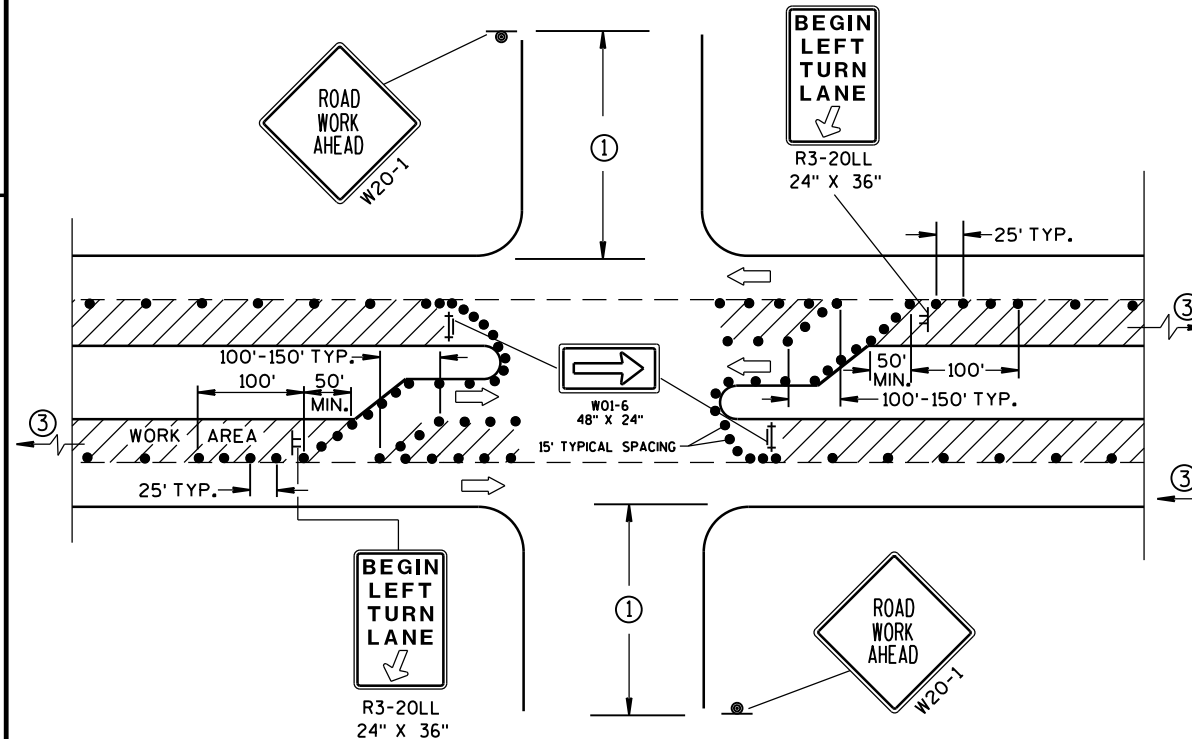
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

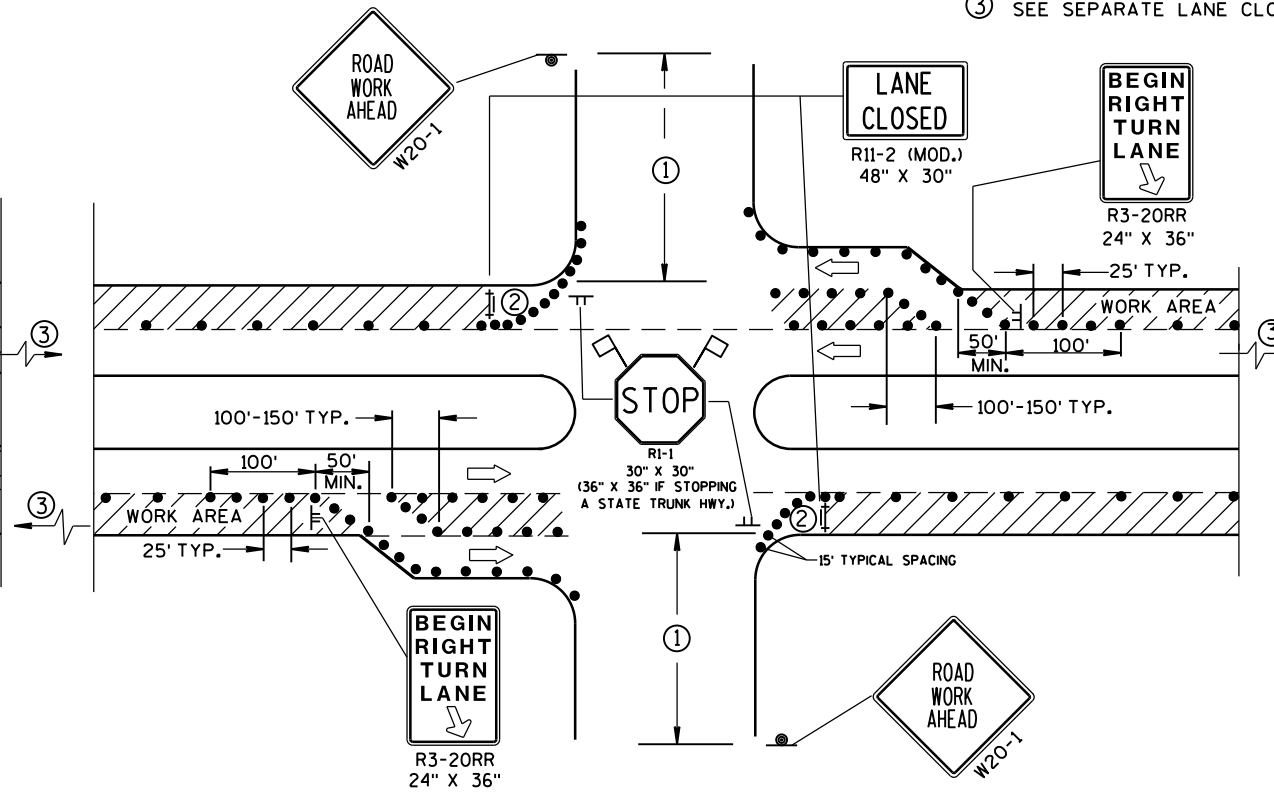
- 500' TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350' IF 35-40 MPH.
200' IF 25-30 MPH.
- ALSO USE BARRICADE AND 15-FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS.
- SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.

LEGEND

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ⊢ SIGN ON TEMPORARY SUPPORT (5' MIN. MOUNTING HEIGHT)
- ⊢ TYPE III BARRICADE WITH ATTACHED SIGN AND TYPE "A" WARNING LIGHT (FLASHING)
- ➡ DIRECTION OF TRAFFIC
- 🚩 FLAGS, 16" X 16" MIN., (ORANGE)
- ▨ WORK AREA



DETAIL C
FOR LEFT LANE CLOSURE AT INTERSECTION OR
MEDIAN OPENING (WITH LEFT TURN BAY OPEN)



DETAIL D
FOR RIGHT LANE CLOSURE AT INTERSECTION
(WITH RIGHT TURN BAY OPEN)

TRAFFIC CONTROL, INTERSECTION WITHIN SINGLE LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

TABLE 1
TAPER AND BUFFER SPACE
FOR 12' LANE WIDTH

s	L	BUFFER SPACE
25	125'	55'
30	180'	85'
35	245'	120'
40	320'	170'
45	540'	220'
50	600'	280'
55	660'	335'

FOR LANE WIDTH OTHER THAN 12':

L = WS AT 45 MPH OR GREATER

$L = \frac{WS^2}{60}$ AT 40 MPH OR LESS

L = TAPER LENGTH IN FEET

S = NON-CONSTRUCTION SPEED LIMIT (MPH)

W = WIDTH OF LANE CLOSURE

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT TWO LANES. FOR CLOSING THE LEFT TWO LANES, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE DISTRICT TRAFFIC UNIT.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

W20-1, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

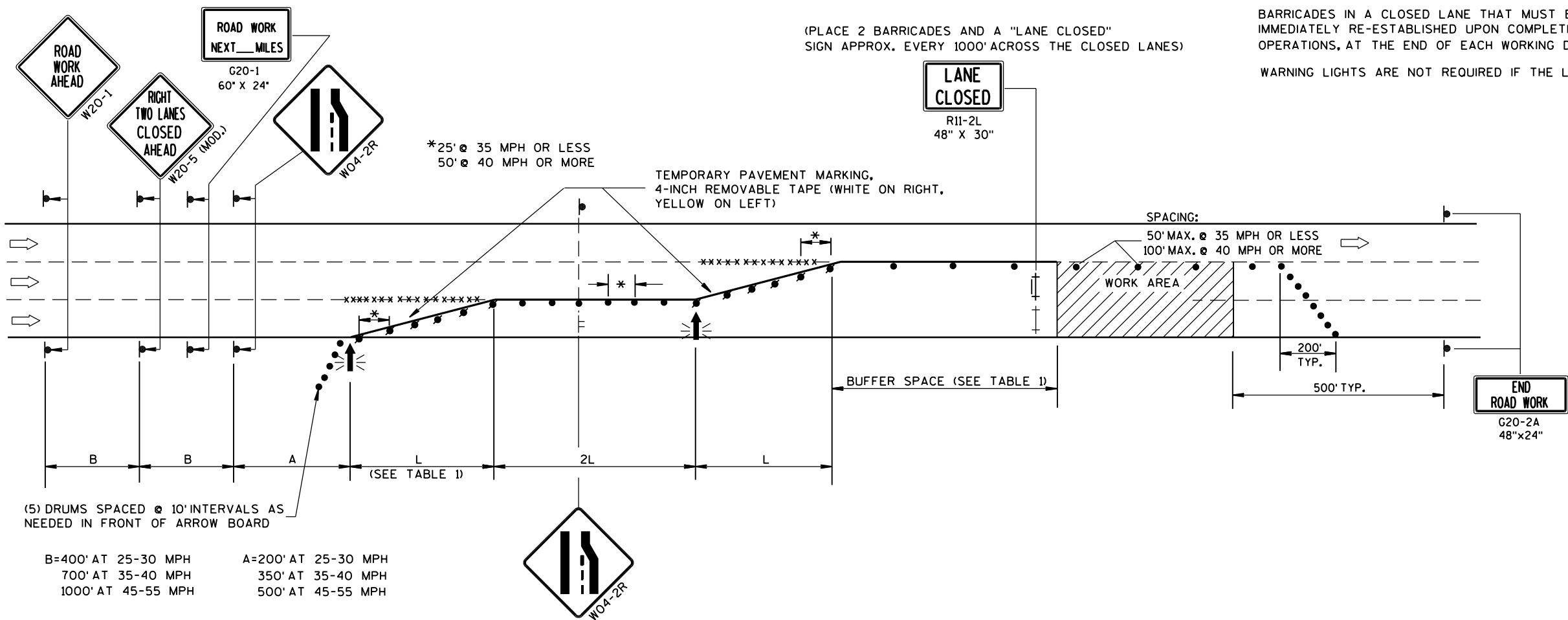
CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROWBOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROWBOARDS AND LANE CLOSURE DRUMS.

WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO LOCATE THE ARROWBOARD AS SHOWN, PLACE THE ARROWBOARD IN THE LANE CLOSURE TAPER AS CLOSE AS POSSIBLE TO THE BEGINNING OF THE TAPER.

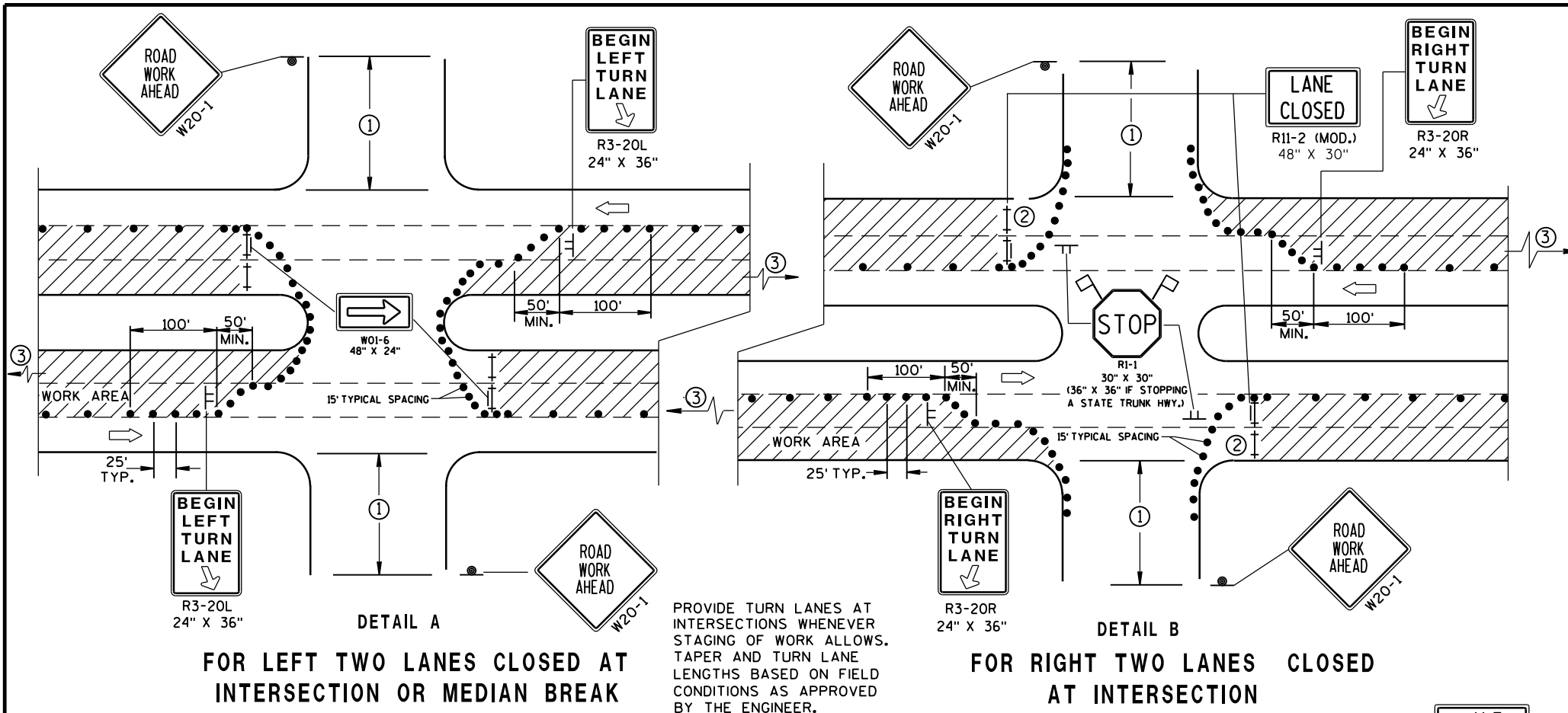
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.



TRAFFIC CONTROL, TWO LANE CLOSURE, NON-FREEWAY/EXPRESSWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY DISTRICT TRAFFIC UNIT.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

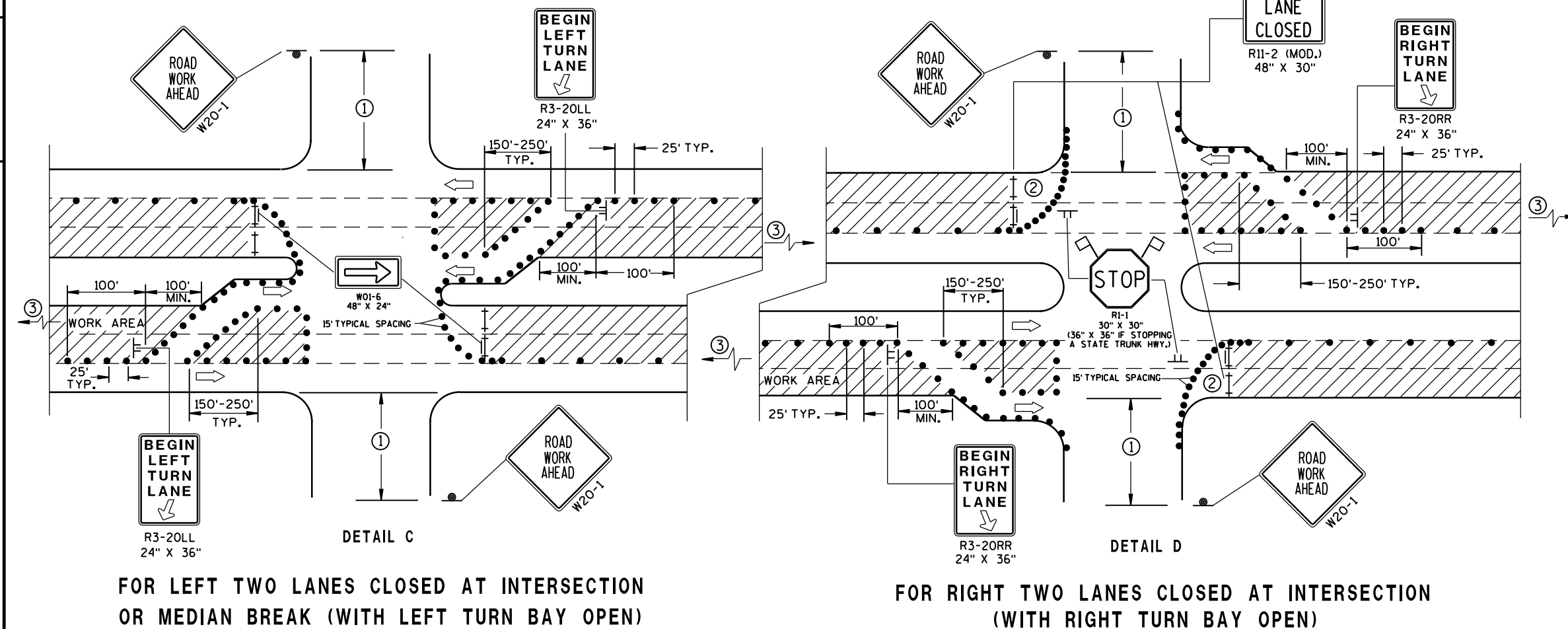
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- ① 500 FEET TYPICAL OR AT LAST INTERSECTION, WHICHEVER IS CLOSER.
350 FEET IF 35-40 MPH.
200 FEET IF 25-30 MPH.
- ② ALSO USE BARRICADES AND 15-FOOT TYPICAL DRUM SPACING AT COMMERCIAL DRIVEWAYS.
- ③ SEE SEPARATE LANE CLOSURE DETAIL FOR ADDITIONAL TRAFFIC CONTROL.

LEGEND

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ⊢ SIGN ON TEMPORARY SUPPORT (5' MIN. MOUNTING HEIGHT)
- ⊢ TYPE III BARRICADE
- ⊢ TYPE III BARRICADE WITH ATTACHED SIGN AND TYPE "A" WARNING LIGHT (FLASHING)
- ➡ DIRECTION OF TRAFFIC
- ⚑ FLAGS, 16" X 16" MIN., (ORANGE)
- ▨ WORK AREA

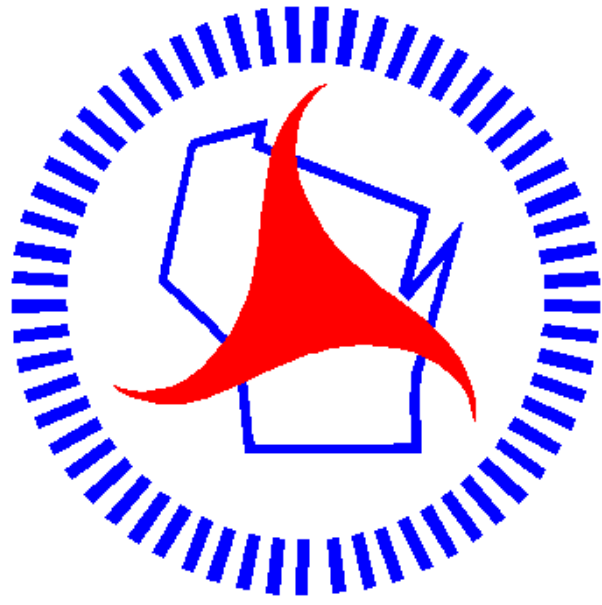


TRAFFIC CONTROL, INTERSECTION WITHIN TWO LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
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