

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

## PLAN OF PROPOSED IMPROVEMENT

## PATTERSON ROAD BRIDGE &amp; APPROACHES

TOWN OF CLARNO

TOWN ROAD

GREEN COUNTY

STATE PROJECT

FEDERAL PROJECT

PROJECT

CONTRACT

5318-00-72

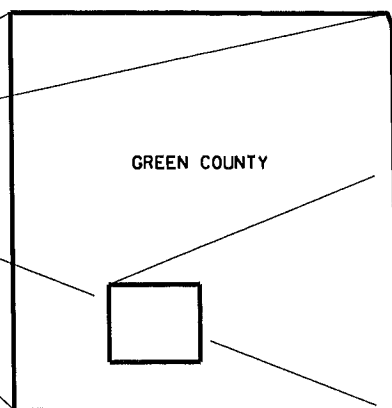
WISC 2015420

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## ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details (Including Erosion Control)
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 40



## DESIGN DESIGNATION

A.D.T. (2015)	=	555
A.D.T. (2035)	=	760
D.H.V.	=	175
D.D.	=	50/50
T.	=	3.9%
DESIGN SPEED	=	35 mph
ESALS	=	51,000

## CONVENTIONAL SYMBOLS

## PLAN

CORPORATE LIMITS

PROPERTY LINE

LOT LINE

LIMITED HIGHWAY EASEMENT

EXISTING RIGHT OF WAY

PROPOSED OR NEW R/W LINE

SLOPE INTERCEPT

REFERENCE LINE

EXISTING CULVERT

PROPOSED CULVERT  
(Box or Pipe)

COMBUSTIBLE FLUIDS

MARSH AREA

WOODED OR SHRUB AREA

## PROFILE

GRADE LINE

ORIGINAL GROUND

MARSH OR ROCK PROFILE  
(To be noted as such)

SPECIAL DITCH

GRADE ELEVATION

CULVERT (Profile View)

UTILITIES

ELECTRIC

FIBER OPTIC

GAS

SANITARY SEWER

STORM SEWER

TELEPHONE

WATER

UTILITY PEDESTAL

POWER POLE

TELEPHONE POLE

HIGH VOLTAGE

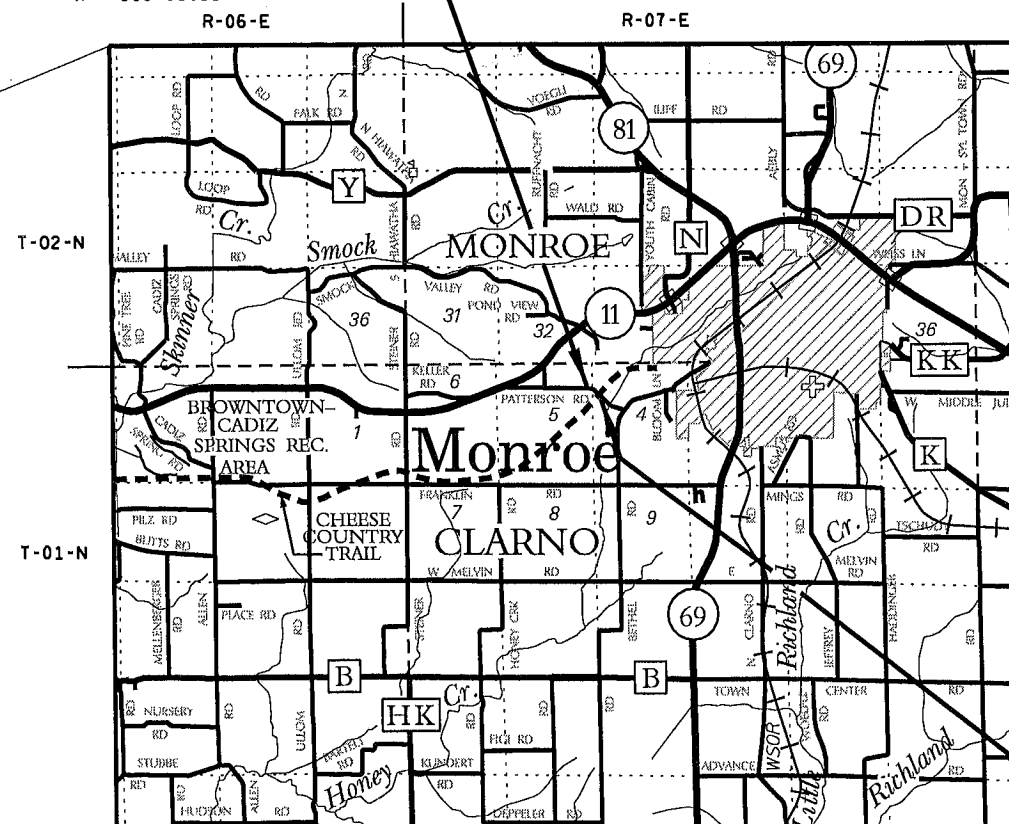
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STATE PROJECT NUMBER

5318-00-72

BEGIN PROJECT

STA 13+75.00

Y = 133895.94  
X = 599798.38

LAYOUT

SCALE 0 1 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.204 MI.

END PROJECT  
STA 24+50.00Coordinates on this plan are referenced to the Wisconsin County  
Coordinate System (WCCS), Green County.

ACCEPTED FOR

County of Green

12/22/2014

(Date)

  
County Highway Commissioner

ACCEPTED FOR

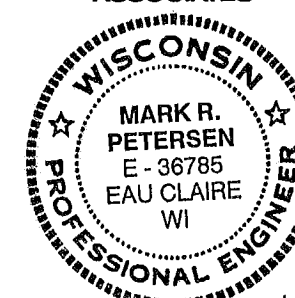
Town of Clarno

12/23/14

(Date)

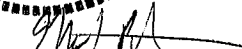
  
Town Chairman

ORIGINAL PLANS PREPARED BY

**AVRES**  
ASSOCIATES

12/17/14

(Date)

  
(Signature)STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor Ayres Associates

Designer Ayres Associates

Management  
Consultant KJohnson Engineers, Inc.

APPROVED FOR REGION OFFICE

DATE: 1/28/15

  
(Management Consultant Signature)

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## GENERAL NOTES

THE EXACT LOCATION AND LIMITS OF FIELD ENTRANCES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

EROSION CONTROL ITEMS TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.

NO TREES ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

SEED MIXTURES NO. 20, NO. 40 AND TEMPORARY SHALL BE USED IN THE PROJECT, AND SHALL BE PLACED AS SHOWN IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER. SEED NO. 40 IS TO BE USED IN LAWN AREAS.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

EXISTING FENCE TO BE REMOVED BY OTHERS.



Dial 811 or (800) 242-8511  
www.DiggersHotline.com

\* DENOTES UTILITIES THAT ARE NOT DIGGERS HOTLINE MEMBERS

## UTILITIES

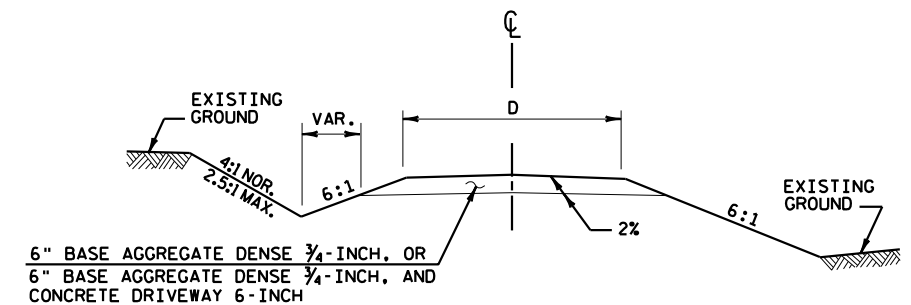
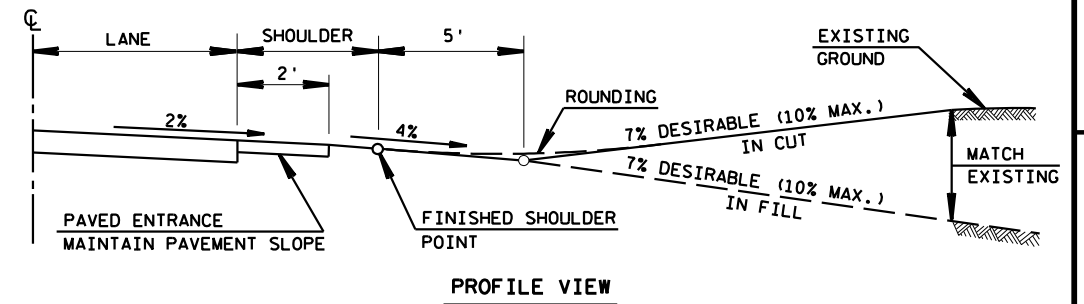
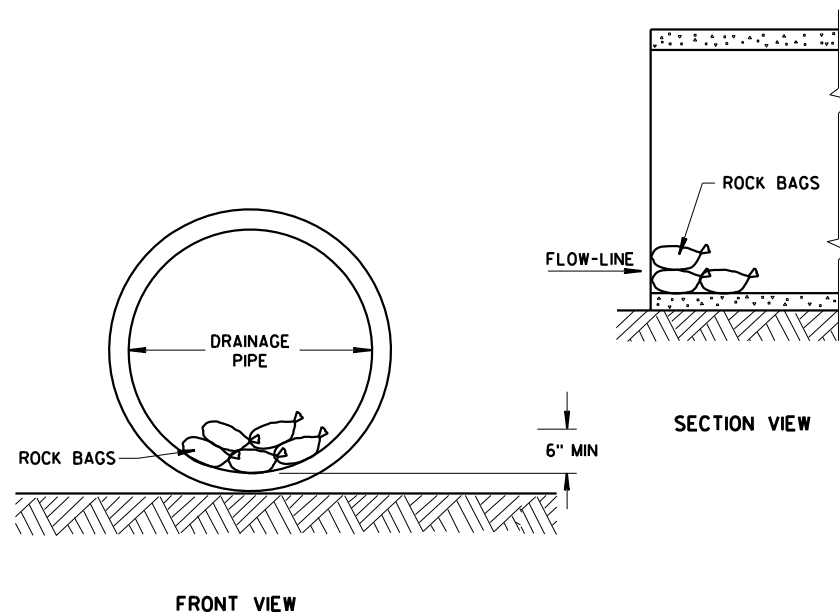
AMERICAN TRANSMISSION COMPANY  
W234 N2000 RIDGEVIEW PARKWAY COURT  
WAUKESHA, WI 53188-1000  
ATTN: TOM BETTHAUSER  
(262) 832-8717  
tbetthouser@atccllc.com

ALLIANT ENERGY  
1915 STATE ROAD 69 SOUTH  
MONROE, WI 53566  
ATTN: STEVE LARSEN  
(608) 328-5339  
stevelarsen@alliantenergy.com

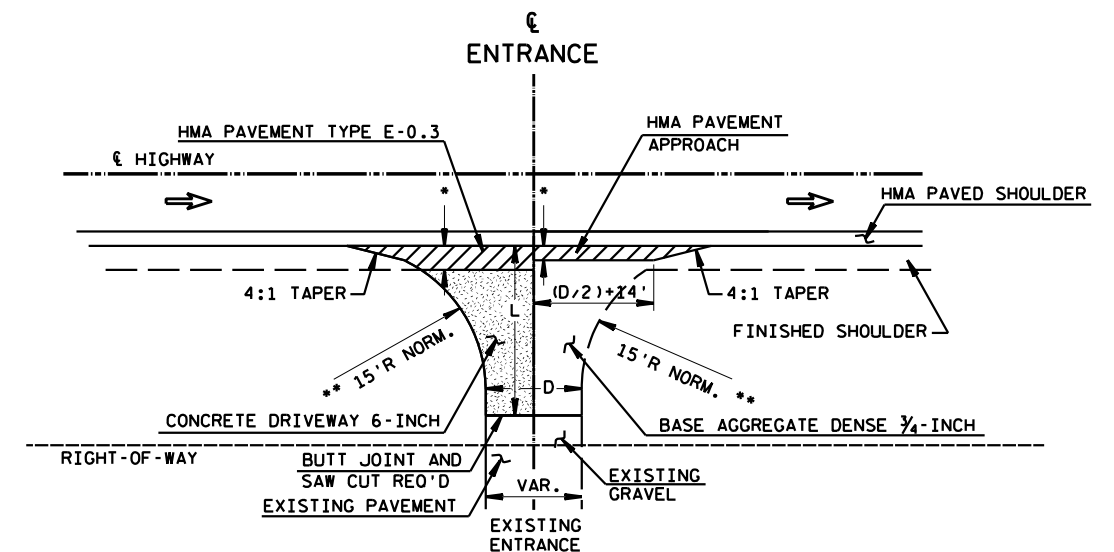
TDS TELECOM  
PO BOX 88  
827 16TH AVENUE  
MONROE, WI 53566  
ATTN: DAREN NEUENSCHWANDER  
(608) 328-1158  
daren.neuenschwander@tdstelecom.com

## CULVERT PIPE CHECKS

(PLACE AT ALL INLET ENDS OF ALL PIPES)



## TYPICAL CROSS SECTION



L=VARIABLE, EXACT LENGTH TO BE DETERMINED IN THE FIELD BY THE ENGINEER. BLEND BACK ON THE ENTRANCE FAR ENOUGH TO GET A SMOOTH PROFILE.

D=DRIVEWAY WIDTH  
D=20'TYP. (PE'S & FE'S) (12'MIN.-22'MAX.)  
D=28'TYP. (CE'S & FARM ENT.) (24'MIN.-35'MAX.)

## PLAN VIEW

RURAL DRIVEWAY INTERSECTION DETAIL  
(PE'S, FE'S & CE'S)

\*\* RADIUS IS TANGENT AT PAVED SHOULDER EDGE OR 5' MAX. OFF EDGE OF MAIN LINE PAVEMENT WHICH EVER IS LESS.  
\* 3' MAX. OR TO FINISHED SHOULDER WHICH EVER IS LESS.

## RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIP-TURF	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPE-TURF			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 2.77 ACRES  
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 2.62 ACRES

PROJECT NO: 5318-00-72

HWY: PATTERSON ROAD

COUNTY: GREEN

TYPICAL SECTIONS: GENERAL NOTES & UTILITIES

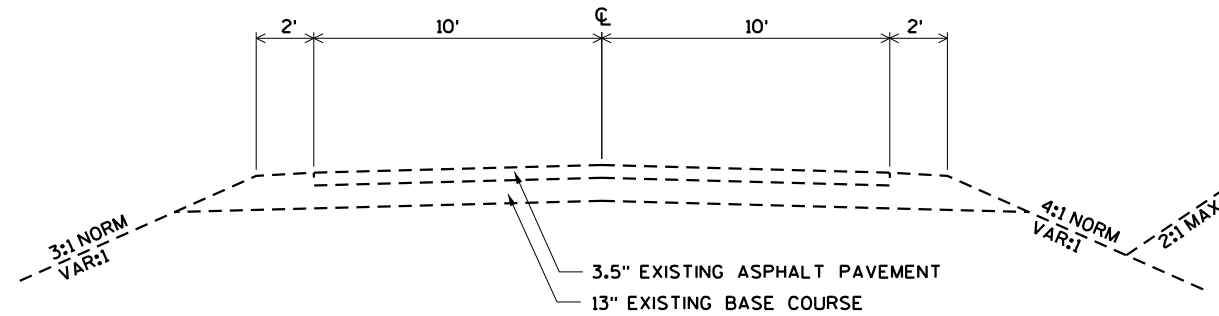
SHEET

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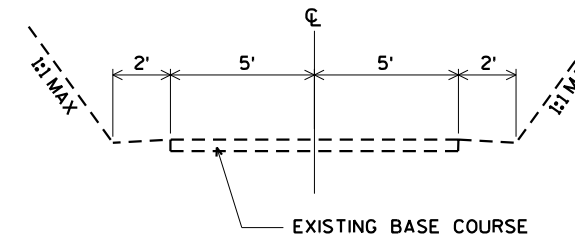
## HMA PAVEMENT LAYER THICKNESS

TOTAL LAYER THICKNESS	LAYERS	ASPHALTIC MATERIAL
4"	1.75" UPPER LAYER 2.25" LOWER LAYER	PG 58-28 PG 58-28

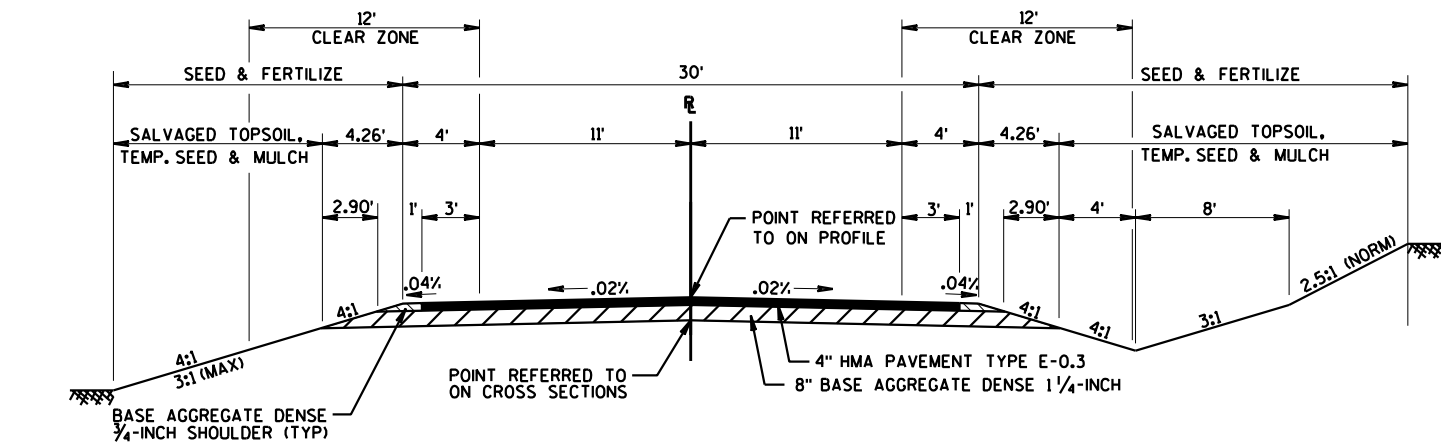
\*\* THE LOW SIDE SHOULDER SLOPE ON SUPERELEVATED SECTION EQUALS THE SUPERELEVATION RATE WHEN THE SUPERELEVATION RATE IS GREATER THAN 4%. IF THE SUPERELEVATION RATE IS LESS THAN OR EQUAL TO 4%, THE LOW SIDE SHOULDER SLOPE IS 4%. HIGH SIDE SHOULDER SLOPE ON SUPERELEVATED SECTIONS EQUALS THE SUPERELEVATION RATE. SEE SUPERELEVATION TABLES FOR SE.



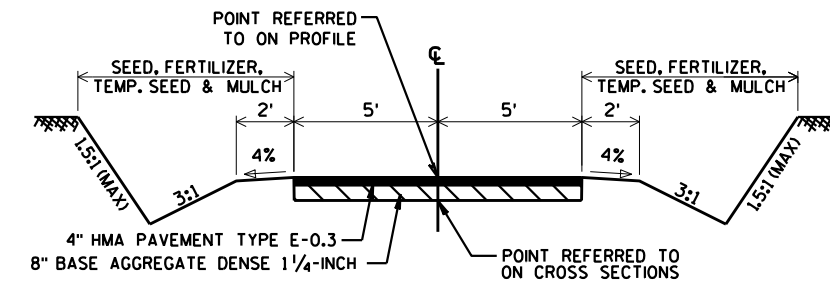
**EXISTING TYPICAL SECTION**  
PATTERSON ROAD



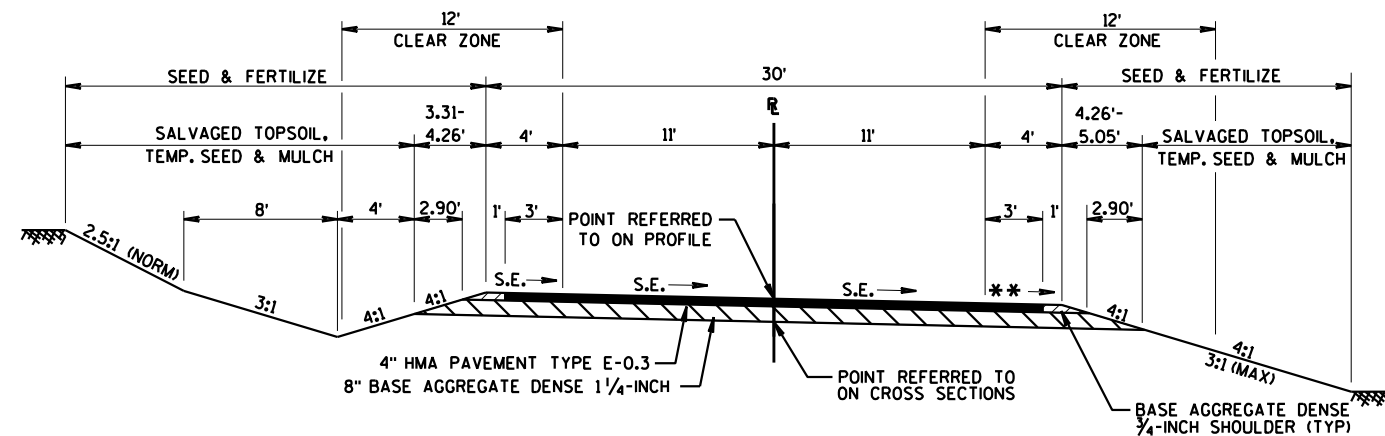
**EXISTING TYPICAL SECTION**  
CHEESE COUNTRY RECREATIONAL TRAIL



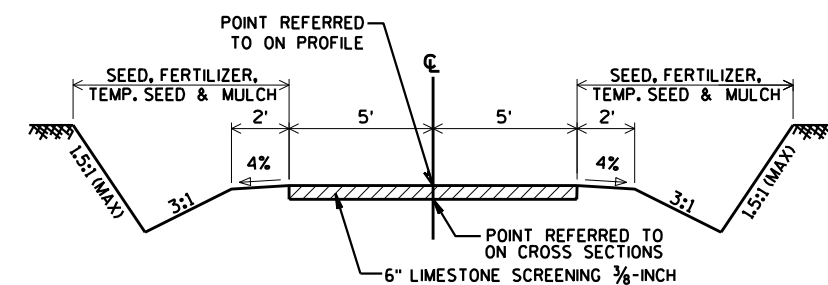
**FINISHED TYPICAL SECTION**  
PATTERSON ROAD



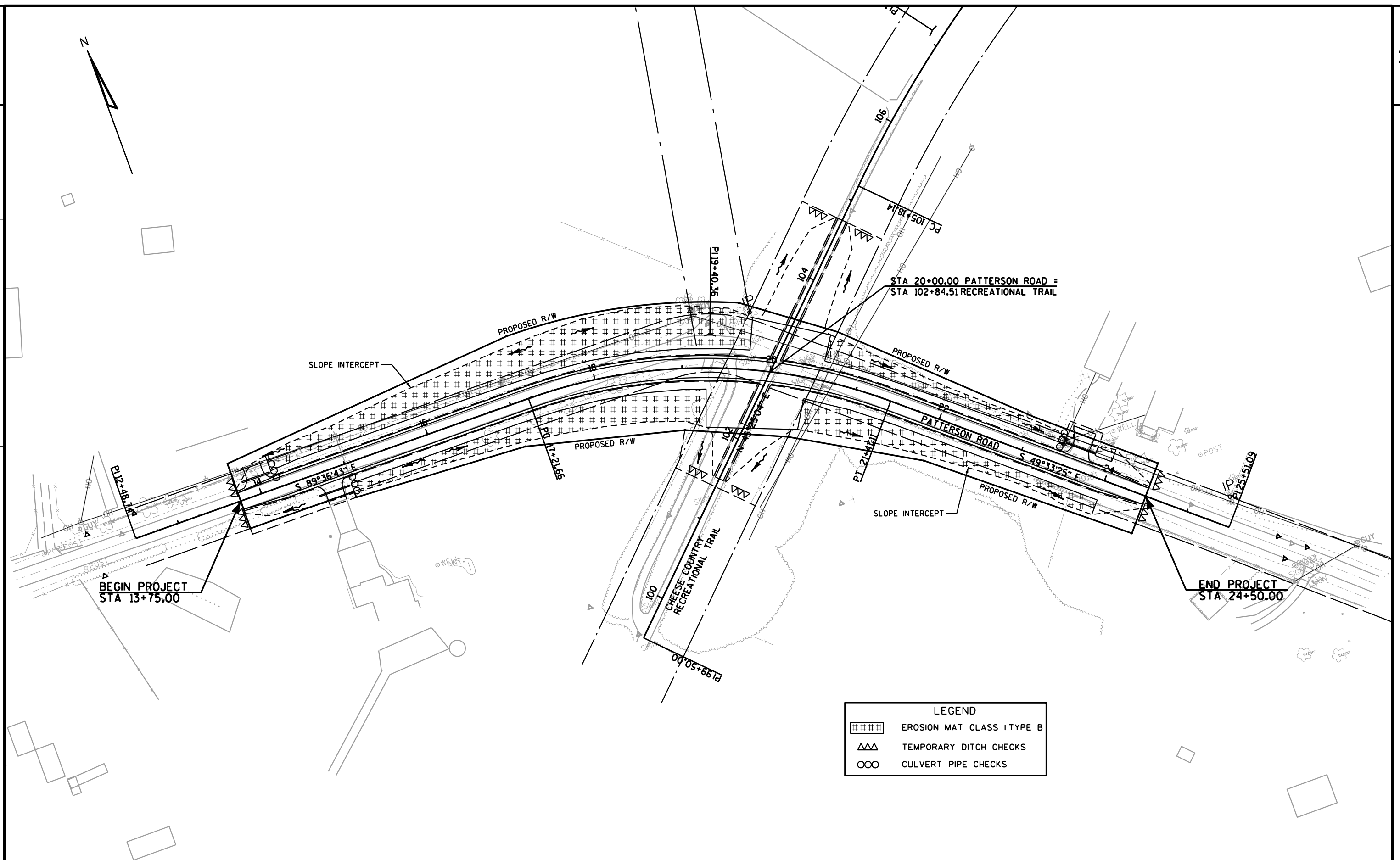
**FINISHED TYPICAL SECTION**  
CHEESE COUNTRY RECREATIONAL TRAIL  
STA 102+18 - STA 102+68  
STA 103+01 - STA 103+51

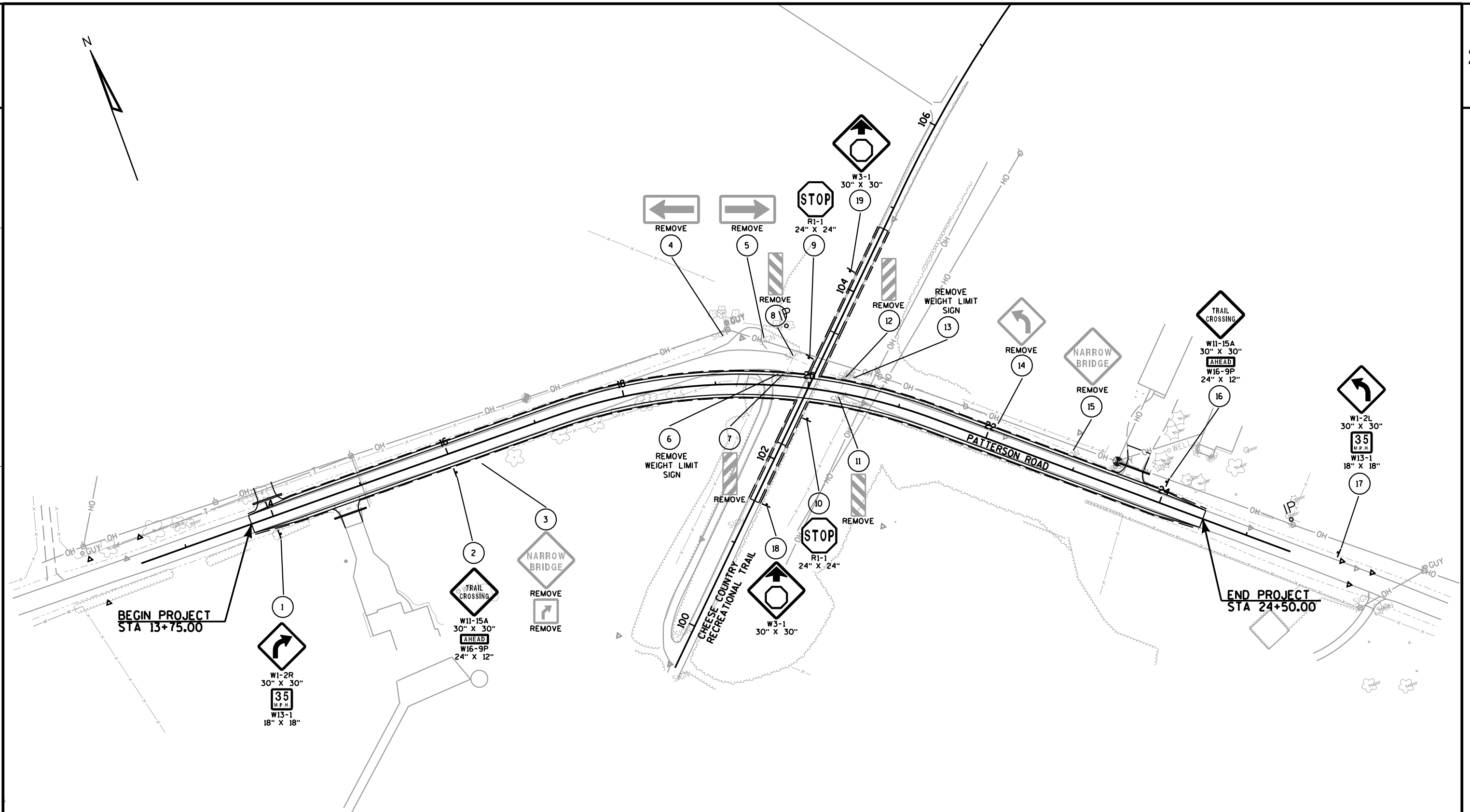


**FINISHED SUPERELEVATED TYPICAL SECTION**  
PATTERSON ROAD



**FINISHED TYPICAL SECTION**  
CHEESE COUNTRY RECREATIONAL TRAIL  
STA 101+50 - STA 102+18  
STA 103+51 - STA 104+75





DATE 27APR15		E S T I M A T E O F Q U A N T I T I E S			
LINE				5318-00-72	
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY
0010	201.0105	Clearing	STA	11.000	11.000
0020	201.0205	Grubbing	STA	11.000	11.000
0030	203.0100	Removing Small Pipe Culverts	EACH	2.000	2.000
0040	203.0200	Removing Old Structure (station) 01. 20+00	LS	1.000	1.000
0050	205.0100	Excavation Common	CY	21,669.000	21,669.000
0060	205.0200	Excavation Rock	CY	5,983.000	5,983.000
0070	213.0100	Finishing Roadway (project) 01. 5318-00-72	EACH	1.000	1.000
0080	305.0110	Base Aggregate Dense 3/4-Inch	TON	130.000	130.000
0090	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,950.000	1,950.000
0100	416.0160	Concrete Driveway 6-Inch	SY	60.000	60.000
0110	455.0105	Asphaltic Material PG58-28	TON	42.900	42.900
0120	455.0605	Tack Coat	GAL	242.000	242.000
0130	460.1100	HMA Pavement Type E-0.3	TON	780.000	780.000
0140	460.2000	Incentive Density HMA Pavement	DOL	500.000	500.000
0150	460.4000	HMA Cold Weather Paving	TON	780.000	780.000
0160	520.0118	Culvert Pipe Class III 18-Inch	LF	108.000	108.000
0170	520.1018	Apron Endwalls for Culvert Pipe 18-Inch	EACH	6.000	6.000
0180	619.1000	Mobilization	EACH	1.000	1.000
0190	624.0100	Water	MGAL	150.000	150.000
0200	625.0500	Salvaged Topsoil	SY	9,261.000	9,261.000
0210	627.0200	Mulching	SY	11,570.000	11,570.000
0220	628.1504	Silt Fence	LF	100.000	100.000
0230	628.1520	Silt Fence Maintenance	LF	100.000	100.000
0240	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0250	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0260	628.2004	Erosion Mat Class I Type B	SY	6,150.000	6,150.000
0270	628.7504	Temporary Ditch Checks	LF	100.000	100.000
0280	628.7555	Culvert Pipe Checks	EACH	8.000	8.000
0290	629.0210	Fertilizer Type B	CWT	7.000	7.000
0300	630.0120	Seeding Mixture No. 20	LB	220.000	220.000
0310	630.0140	Seeding Mixture No. 40	LB	90.000	90.000
0320	630.0200	Seeding Temporary	LB	155.000	155.000
0330	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	8.000	8.000
0340	637.2210	Signs Type II Reflective H	SF	6.620	6.620
0350	637.2230	Signs Type II Reflective F	SF	46.000	46.000
0360	638.2602	Removing Signs Type II	EACH	12.000	12.000
0370	638.3000	Removing Small Sign Supports	EACH	12.000	12.000
0380	642.5001	Field Office Type B	EACH	1.000	1.000
0390	643.0100	Traffic Control (project) 01. 5318-00-72	EACH	1.000	1.000
0400	650.4500	Construction Staking Subgrade	LF	1,400.000	1,400.000
0410	650.5000	Construction Staking Base	LF	1,400.000	1,400.000
0420	650.9910	Construction Staking Supplemental Control (project) 01. 5318-00-72	LS	1.000	1.000
0430	650.9920	Construction Staking Slope Stakes	LF	1,400.000	1,400.000
0440	690.0150	Sawing Asphalt	LF	40.000	40.000
0450	690.0250	Sawing Concrete	LF	20.000	20.000
0460	SPV.0195	Special 01. Limestone Screenings 3/8-Inch	TON	75.000	75.000

CLEARING AND GRUBBING			
STATION TO STATION	LOCATION	201.0105	201.0205
		CLEARING STA	GRUBBING STA
13+75 - 15+00	LT/RT	2	2
16+00 - 24+50	LT/RT	9	9
PROJECT TOTALS		11	11

REMOVING SMALL PIPE CULVERTS					
STATION	LOCATION	203.0100	LENGTH	SIZE	COMMENTS
		EACH			
14+79	RT	1	29'	15"	DRIVEWAY CMCP
23+62	LT	1	38'	12"	DRIVEWAY CMCP
PROJECT TOTAL		2			

REMOVING OLD STRUCTURE (STA 20+00)		
STATION	203.0200	COMMENT
	LS	
20+00	1	P-23-0174

EARTHWORK SUMMARY											
DIVISION	STATION TO STATION	LOCATION	CUT CY	205.0100	(9)	(6)	UNEXPANDED FILL CY	(2)	(4)	(5)	(1)
				EXCAVATION COMMON CY	AVAILABLE MATERIAL CY	205.0200 EXCAVATION ROCK CY		EXPANDED FILL CY	EXPANDED FILL CY	MASS ORDINATE +/- CY	WASTE CY
	13+75 - 24+50	PATTERSON RD	25191	20244	20244	4947	172	215	25471	25471	
	101+50 - 104+75	TRAIL	2461	1425	1425	1036	175	219	2346	2346	
PROJECT ID TOTALS				21669		5983					

(1) WASTE = EXPANDED FILL - EXCAVATION COMMON - 1.1 \* EXCAVTION ROCK (FOR INFORMATION ONLY - NOT A BID ITEM)  
(2) EXPANSION FACTOR = 1.25  
(3) BORROW = EXPANDED FILL -AVAILABLE MATERIAL  
(4) EXPANDED FILL = UNEXPANDED FILL \* EXPANSION FACTOR  
(5) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION.  
MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.  
(6) ROCK EXPANSION FACTOR = 1.1  
(9) AVAILABLE MATERIAL = COMMON EXCAVATION

BASE AGGREGATE DENSE		
STATION TO STATION	305.0110	305.0120
	3/4-INCH TON	1 1/4-INCH TON
13+75 - 24+50	90	1900
102+18 - 102+68	---	25
103+01 - 103+51	---	25
DRIVEWAYS	40	---
PROJECT TOTALS		130 1950

CONCRETE DRIVEWAY 6-INCH		
STATION	LOCATION	416.0160 SY
14+79	RT	60
PROJECT TOTAL		60

FINISHING ROADWAY	
LOCATION	213.0100 EACH
01. 5318-00-72	1
PROJECT TOTAL	
	1

HMA PAVEMENT ITEMS					
STATION TO STATION	LOCATION	455.0105	460.1100	460.4000	
		ASPHALTIC MATERIAL PG58-28 TON	455.0605 TACK COAT GAL	HMA PAVEMENT TYPE E-0.3 TON	HMA COLD WEATHER PAVING TON
13+75 - 24+50	MAINLINE	41.3	234	750	750
102+18 - 102+68	TRAIL	0.8	4	15	15
103+01 - 103+51	TRAIL	0.8	4	15	15
PROJECT TOTALS		42.9	242	780	780

CULVERT PIPE CLASS III 18-INCH					
STATION	LOCATION	520.0118	520.1018	THICKNESS(INCHES)	
		PIPE LF	ENDWALL EACH	STEEL	ALUMINUM
14+00	LT	36	2	0.064	0.060
14+79	RT	36	2	0.064	0.060
23+62	LT	36	2	0.064	0.060
PROJECT TOTALS		108	6		

SALVAGED TOPSOIL, MULCHING, FERTILIZER, & SEEDING							
STATION TO STATION	LOCATION	625.0500	627.0200	629.0210	630.0120	630.0140	630.0200
		SALVAGED TOPSOIL SY	MULCHING SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 20 LB	SEEDING MIXTURE NO. 40 LB	SEEDING TEMPORARY LB
13+75 - 24+50	PATTERSON RD	8307	8307	5	150	74	112
101+50 - 104+75	TRAIL	954	954	1	26	---	13
UNDISTRIBUTED		---	2309	1	44	16	30
PROJECT TOTALS		9261	11570	7	220	90	155

CULVERT PIPE CHECKS		
STATION	LOCATION	628.7555 EACH
14+00	LT	2
14+79	RT	2
23+62	LT	2
UNDISTRIBUTED		2
PROJECT TOTAL		8

FIELD OFFICE TYPE B	
PROJECT	642.5001 EACH
ENTIRE PROJECT	1
PROJECT TOTAL	1

TRAFFIC CONTROL (5318-00-72)	
PROJECT	643.0100 EACH
01. 5318-00-72	1
PROJECT TOTAL	1

MOBILIZATION	
LOCATION	619.1000 EACH
ENTIRE PROJECT	1
PROJECT TOTAL	1

WATER	
LOCATION	624.0100 MGAL
BASE COMPACTION	50
DUST CONTROL	100
PROJECT TOTAL	150

SILT FENCE		
LOCATION	628.1504 LF	628.1520 MAINTENANCE LF
UNDISTRIBUTED	100	100
PROJECT TOTALS	100	100

MOBILIZATIONS EROSION CONTROL		
LOCATION	628.1905 EACH	628.1910 EMERGENCY EACH
ENTIRE PROJECT	2	2
PROJECT TOTAL	2	2

SIGN NO.	STATION	SIGN CODE	SIZE	634.0614	637.2210	637.2230	MESSAGE
				POSTS WOOD 4X6-INCH 14-FT EACH	SIGNS TYPE II REFLECTIVE H SF	SIGNS TYPE II REFLECTIVE F SF	
1	14+00, RT	W1-2R	30 X 30	1	---	6.25	"35"
2	16+00, RT	W13-1	18 X 18	---	---	2.25	
		W11-15A	30 X 30	1	---	6.25	
9	103+20, LT	W16-9P	24 X 12	---	---	2.00	
	102+49, RT	R1-1	24 X 24	1	3.31	---	
10	102+49, RT	R1-1	24 X 24	1	3.31	---	
16	24+00, LT	W11-15A	30 X 30	1	---	6.25	
17	26+00, LT	W16-9P	24 X 12	---	---	2.00	
		W1-2L	30 X 30	1	---	6.25	
18	101+49, RT	W13-1	18 X 18	---	---	2.25	
		W3-1	30 X 30	1	---	6.25	
19	104+20, LT	W3-1	30 X 30	1	---	6.25	
PROJECT TOTALS				8	6.62	46.00	

REMOVING SIGNS TYPE II & REMOVING SMALL SIGN SUPPORTS			
SIGN NO.	STATION	638.2602 SIGNS EACH	638.3000 SUPPORTS EACH
3	16+25, RT	2	1
4	19+00, LT	1	1
5	19+50, LT	1	1
6	19+75, LT	1	1
7	19+75, LT	1	1
8	19+80, LT	1	1
11	20+25, RT	1	1
12	20+30, LT	1	1
13	20+30, LT	1	2
14	22+00, LT	1	1
15	23+00, LT	1	1
PROJECT TOTALS		12	12

CONSTRUCTION STAKING			
STATION TO STATION	650.4500 SUBGRADE LF	650.5000 BASE LF	650.9920 SLOPE STAKES LF
13+75 - 24+50	1075	1075	1075
101+50 - 104+75	325	325	325
PROJECT TOTALS	1400	1400	1400

EROSION MAT CLASS I TYPE B		
STATION TO STATION	LOCATION	628.2004 SY
14+00 - 19+75	LT	2453
15+00 - 19+25	RT	1069
20+43 - 24+00	RT	851
20+58 - 24+00	LT	575
UNDISTRIBUTED		1202
PROJECT TOTAL		6150

TEMPORARY DITCH CHECKS		
STATION	LOCATION	628.7504 LF
13+75	LT/RT	20
24+50	LT/RT	20
101+50	LT/RT	20
104+75	LT/RT	20
UNDISTRIBUTED		20
PROJECT TOTAL		100

CONSTRUCTION STAKING SUPPLEMENTAL CONTROL	
PROJECT	650.9910 LS
01. 5318-00-72	1

SAWING			
STATION	LOCATION	690.0150 ASPHALT LF	690.025 CONCRETE LF
13+75	CL	20	---
14+79	RT	---	20
24+50	CL	20	---
PROJECT TOTALS		40	20

LIMESTONE SCREENING 3/8-INCH	
STATION TO STATION	SPV.0195.01 TON
101+50 - 102+18	25
103+51 - 104+75	50
PROJECT TOTAL	75



CONVENTIONAL ABBREVIATIONS			
ACCESS POINT/ DRIVEWAY CONNECTION	AP	RECORDED AS	(100')
ACCESS RIGHTS	AR	REFERENCE LINE	R/L
ACRES	AC.	RELEASE OF RIGHTS	ROR
AND OTHERS	ET.AL.	REMAINING	REM.
CENTERLINE	C/L	RIGHT-OF-WAY	R/W
CERTIFIED SURVEY MAP	CSM	SECTION	SEC.
CORNER	COR.	STATION	STA.
DOCUMENT	DOC.	TEMPORARY LIMITED EASEMENT	TLE
EASEMENT	EASE.	VOLUME	V.
HIGHWAY EASEMENT	H.E.	CURVE DATA	
LAND CONTRACT	LC	LONG CHORD	LCH
MONUMENT	MON.	LONG CHORD BEARING	LCB
PAGE	P.	RADIUS	R
PERMANENT LIMITED EASEMENT	PLE	DEGREE OF CURVE	D
PRIVATE DRIVE	P.D.	CENTRAL ANGLE OR DELTA	Δ
PROPERTY LINE	PL	LENGTH OF CURVE	L
		TANGENT	T

CONVENTIONAL UTILITY SYMBOLS			
WATER	— W —		
GAS	— G —		
TELEPHONE	— T —		
OVERHEAD	— OH —		
TRANSMISSION LINES	— E —		
ELECTRIC	— TV —		
CABLE TELEVISION	— FO —		
FIBER OPTIC	— SAN —		
SANITARY SEWER	— SS —		
STORM SEWER			
NON COMPENSABLE			
POWER POLE			
TELEPHONE POLE			
TELEPHONE PEDESTAL			
ELECTRIC TOWER			

CONVENTIONAL SYMBOLS			
SECTION LINE	---	SECTION CORNER	5 4 3 2 1
QUARTER LINE	---		
SIXTEENTH LINE	---		
NEW REFERENCE LINE	---	NOTATION FOR COMBUSTIBLE FLUIDS	CAUTION
NEW R/W LINE	---		
EXISTING R/W LINE	---	NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES	CAUTION
PROPERTY LINE	---		
LOT, TIE AND OTHER MINOR LINES	---		
CORPORATE LIMITS	---		
FEE ACQUISITION AREA (HATCHING VARIES BY OWNER)	---	ELECTRIC POLE	
TEMPORARY LIMITED EASEMENT AREA	---	TELEPHONE POLE	
EASEMENT (HIGHWAY, PERMANENT LIMITED OR RESTRICTED DEVELOPMENT) AREAS	---	PEDESTAL (LABEL TYPE - (COMMUNICATIONS, ELECTRIC))	
BUILDING	---		
		NO ACCESS (BY PARCEL ACQUISITION)	
		NO ACCESS (BY STATUTORY AUTHORITY)	
		ACCESS RESTRICTED (BY PREVIOUS PROJECT/CONTROL)	
		COMPENSABLE	
		NON-COMPENSABLE	
		PARCEL NUMBER	6
		UTILITY PARCEL NUMBER	83

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, GREEN COUNTY, NAD83(1991) ADJUSTMENT IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS AND GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT BY OTHERS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD".

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY LINES, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:  
EXISTING HIGHWAY R/W PATTERSON ROAD ESTABLISHED FROM CENTERLINE OF EXISTING ROAD AND STATUTE 82.31.

A **TEMPORARY LIMITED EASEMENT** (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. ALL TLES EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

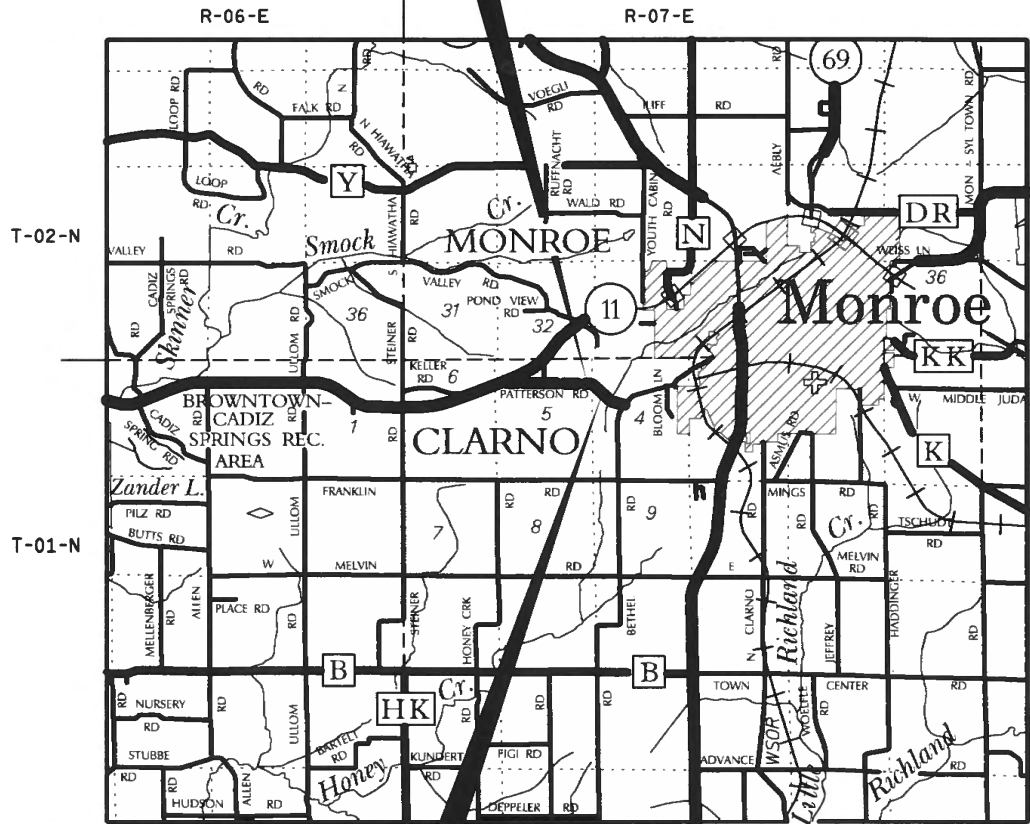
A **HIGHWAY EASEMENT** (H.E.) IS AN EASEMENT FOR HIGHWAY PURPOSES, AS LONG AS SO USED, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO NEW REFERENCE LINES.

BEGIN RELOCATION ORDER

STA. 13+75.00

1,445.28' SOUTH OF AND 538.43' WEST OF THE NORTHEAST CORNER OF SEC. 5, T. 01N., R. 07 E.



END RELOCATION ORDER

STA. 24+50.00

1,791.36' SOUTH OF AND 428.45' EAST OF THE NORTHEAST CORNER OF SEC. 5, T. 01N., R. 07 E.

LAYOUT  
SCALE 0 1 MI.

TOTAL NET LENGTH OF CENTERLINE = 0.204 MI.

R/W PROJECT NUMBER	5318-00-02	SHEET NUMBER	4.01	TOTAL SHEETS	2
FEDERAL PROJECT NUMBER					
PLAT OF RIGHT-OF-WAY REQUIRED FOR <b>PATTERSON ROAD BRIDGE &amp; APPROACHES, TOWN OF CLARNO</b>					
TOWN ROAD		GREEN COUNTY			
CONSTRUCTION PROJECT NUMBER	5318-00-72				

ACCEPTED FOR  
Town of Clarno  
9/17/14  
(Date) Matthew Hartung  
Town Chairman



THIS SURVEY IS PREPARED AT THE REQUEST OF THE TOWN. THE FIELD SURVEY WAS PERFORMED IN JULY 2009. THIS SURVEY IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

(SIGNATURE) John C. Favorite  
DATE: 9/12/14  
(PRINTED NAME) JOHN FAVORITE  
(REGISTRATION NUMBER) S-1890

REVISION DATE

# SCHEDULE OF LANDS & INTERESTS REQUIRED

ALL AREAS SHOWN IN ACRES UNLESS OTHERWISE NOTED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TOTAL ACRES	NEW	R/W ACRES OR EXISTING	S.F. TOTAL	TOTAL ACRES REM.	TLE ACRES OR S.F.	H.E. ACRES
1	DANA & VICKI VALLERO	FEE	20.00	0.37	0.42	0.79	19.21	---	---
2	FIRST BANKING CENTER	FEE	1.91	0.02	0.04	0.06	1.85	---	---
3	WILLIAM L. & SHARON H. SUTTER	FEE	75,359 S.F.	16 S.F.	---	16 S.F.	75,343 S.F.	---	---
4	PECATONICA RAIL TRANSIT COMMISSION	H.E. & TLE	---	---	---	---	---	0.44	0.18
6	KENNETH L. SCHINDLER	FEE & TLE	16.13	0.10	0.30	0.40	15.73	0.01	---
7	ROBERTA R. KRUEGER	FEE	7.04	0.19	0.30	0.49	6.55	---	---
8	GREGORY E. & JULIE A. KNOKE	FEE & TLE	5.59	0.26	0.43	0.69	4.90	1,040 S.F.	---
81	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS	---	---	---	---	---	---	---
82	WISCONSIN POWER & LIGHT	RELEASE OF RIGHTS	---	---	---	---	---	---	---
83	TDS EASEMENT RELEASE	RELEASE OF RIGHTS	---	---	---	---	---	---	---

AREAS SHOWN IN THE TOTAL ACRES COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM TAX ROLLS OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE ACQUIRED.

(83) TDS EASEMENT RELEASE  
V. 266 / P. 361 / DOC. 243100 - BLANKET EASEMENT  
ACROSS GOV. LOT 1, SEC. 5, T1N, R7E - PARCEL 1

V. 270 / P. 13 / DOC. 244402 - BLANKET EASEMENT  
ACROSS GOV. LOT 5, SEC. 4, T1N, R7E - PARCEL 6

GOVERNMENT  
LOT 1  
5-01-07

## COURSE TABLE

COURSE	BEARING	DISTANCE
101-102	S 53°33'25" E	117.35'
101-120	S 53°33'25" E	16.11'
T20-102	S 53°33'25" E	101.24'
103-104	S 49°33'25" E	150.00'
103-T40	S 49°33'25" E	40.00'
T40-T41	N 40°26'35" E	10.00'
T41-T42	S 49°33'25" E	40.00'
T42-T43	S 40°26'35" W	10.00'
T43-T40	N 49°33'25" W	40.00'
110-111	N 66°14'02" W	60.74'
111-112	N 66°14'02" W	56.46'
111-100	N 00°31'48" W	158.94'

R.= 675.00'  
L.C.B.= S 80°47'38" E  
L.C.= 206.95'  
L.= 207.77'

BRASS CAPPED  
CONC. MON.  
Y 135,340.459  
X 600,337.640

## CHEESE COUNTRY RECREATION TRAIL REFERENCE LINE

PI = 107+13.76  
Y = 134,128.905  
X = 600,719.615  
R = 1428.00'  
Delta = 15°36'04" RT  
D = 4°00'44"  
L = 388.83'  
T = 195.63'  
PC = 105+18.14  
Y = 133,991.588  
X = 600,580.282  
PT = 109+06.97  
Y = 134,223.690  
X = 600,890.744  
BK = N 45°25'04" E  
AH = N 61°01'08" E

## BEGIN RELOCATION ORDER

STA. 13+75

Y=133,895.177  
X=599,799.208

Station	Offset
100	19+06.34
101	19+60.00
102	103+58.31
103	20+64.67
104	23+00.00
105	24+50.00
106	24+50.00
107	24+50.00
108	24+50.00
109	22+00.00
110	20+32.95
111	102+04.15
112	19+00.00
113	17+00.00
114	15+00.00
115	15+00.00
116	13+75.00
117	13+75.00
118	13+75.00
119	13+75.00
120	17+21.66
T20	104+50.00
T21	104+75.00
T22	104+75.00
T23	104+75.00
T31	101+49.53
T32	101+40.00
T33	101+40.00
T34	101+40.00
T35	101+50.00

## PATTERSON ROAD

PI = 19+40.37  
Y = 133,891.347  
X = 600,364.559  
R = 600.00'  
Delta = 40°03'18" RT  
D = 9°32'57"  
L = 419.46'  
T = 218.71'  
PC = 17+21.66  
Y = 133,892.829  
X = 600,145.855  
PT = 21+41.11  
Y = 133,749.473  
X = 600,531.007  
BK = S 89°36'43" E  
AH = S 49°33'25" E

## TOWN

DANA & VICKI VALLERO

(1)

(82) WISCONSIN POWER & LIGHT

(120)

R.= 675.00'  
L.C.B.= S 80°47'38" E  
L.C.= 206.95'  
L.= 207.77'

## OF

STA 19+31.70

(2) FIRST BANKING CENTER

OUTLOT 1

WILLIAM L. & SHARON H. SUTTER

(3)

GOVERNMENT  
LOT 4  
4-01-07

## CLARNO

(81) AMERICAN TRANSMISSION COMPANY  
EASEMENT RELEASE - 40' EITHER SIDE  
OF DEFINED REFERENCE LINE  
V. 24 / P. 506 / DOC. 179892 - PARCEL 6  
V. 25 / P. 8 / DOC. 179996 - PARCEL 7

POINT	Y	X
100	133,934.719	600,350.646
101	133,913.500	600,407.139
102	133,843.789	600,501.544
103	133,676.847	600,677.875
104	133,579.543	600,792.033
105	133,574.215	600,787.492
106	133,549.101	600,766.085
107	133,523.986	600,744.678
108	133,514.853	600,736.894
109	133,673.221	600,543.388
110	133,751.308	600,407.704
111	133,775.786	600,352.116
112	133,798.539	600,300.446
113	133,842.977	600,123.859
114	133,854.331	599,923.931
115	133,861.331	599,923.978
116	133,862.177	599,798.984
118	133,928.176	599,799.431
119	133,940.176	599,799.512
120	133,967.827	600,146.363

## END RELOCATION ORDER

STA. 24+50.00

Y=133,549.101  
X=600,766.085

LOT 2  
C.S.M. 3952  
V. 15, PGS. 218-219, DOC. 472176

REVISION DATE

DATE 9/12/14

SCALE, FEET

0 50 100

HWY: PATTERSON ROAD

COUNTY: GREEN

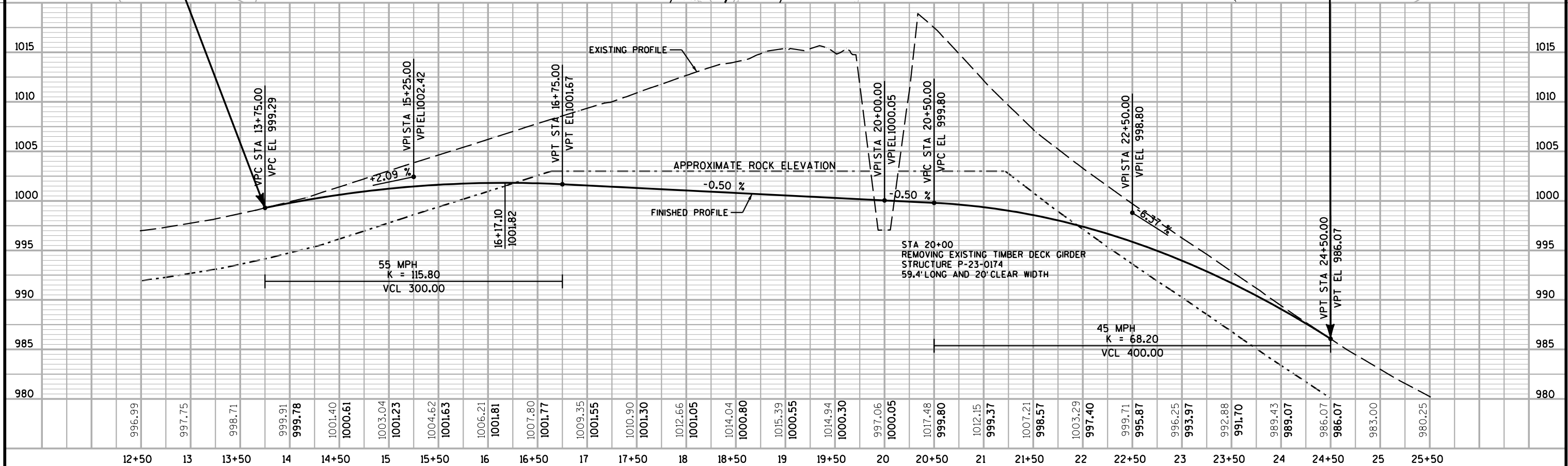
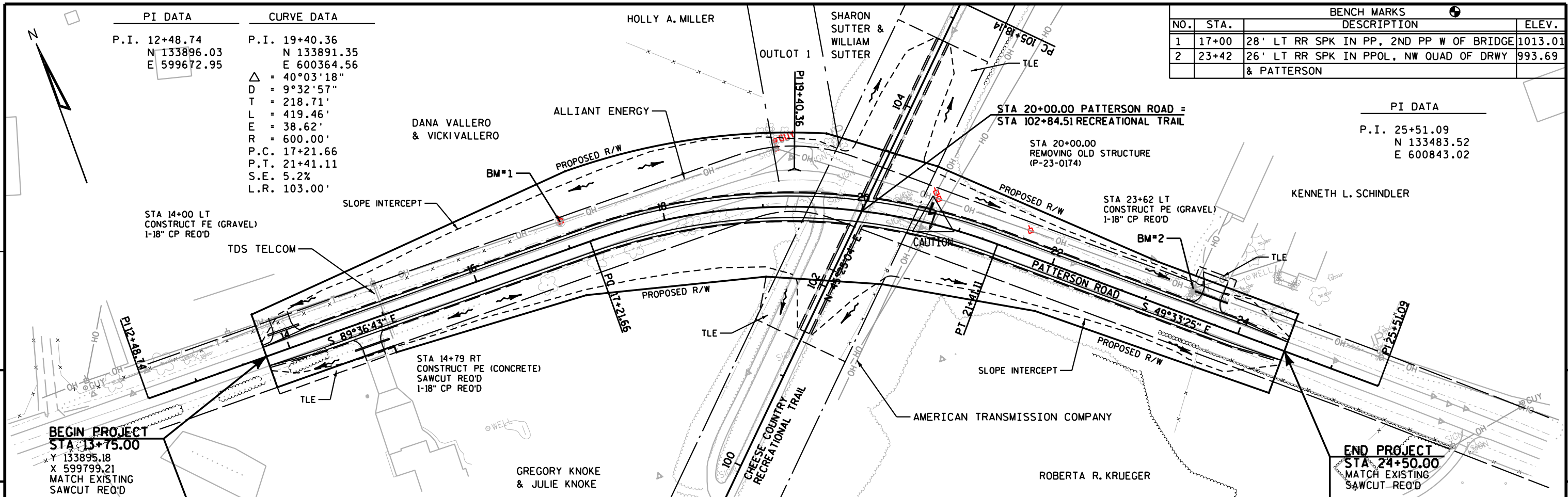
R/W PROJECT NUMBER 5318-00-02

CONSTRUCTION PROJECT NUMBER 5318-00-72

PLAT SHEET 4.02

PS&E SHEET

E



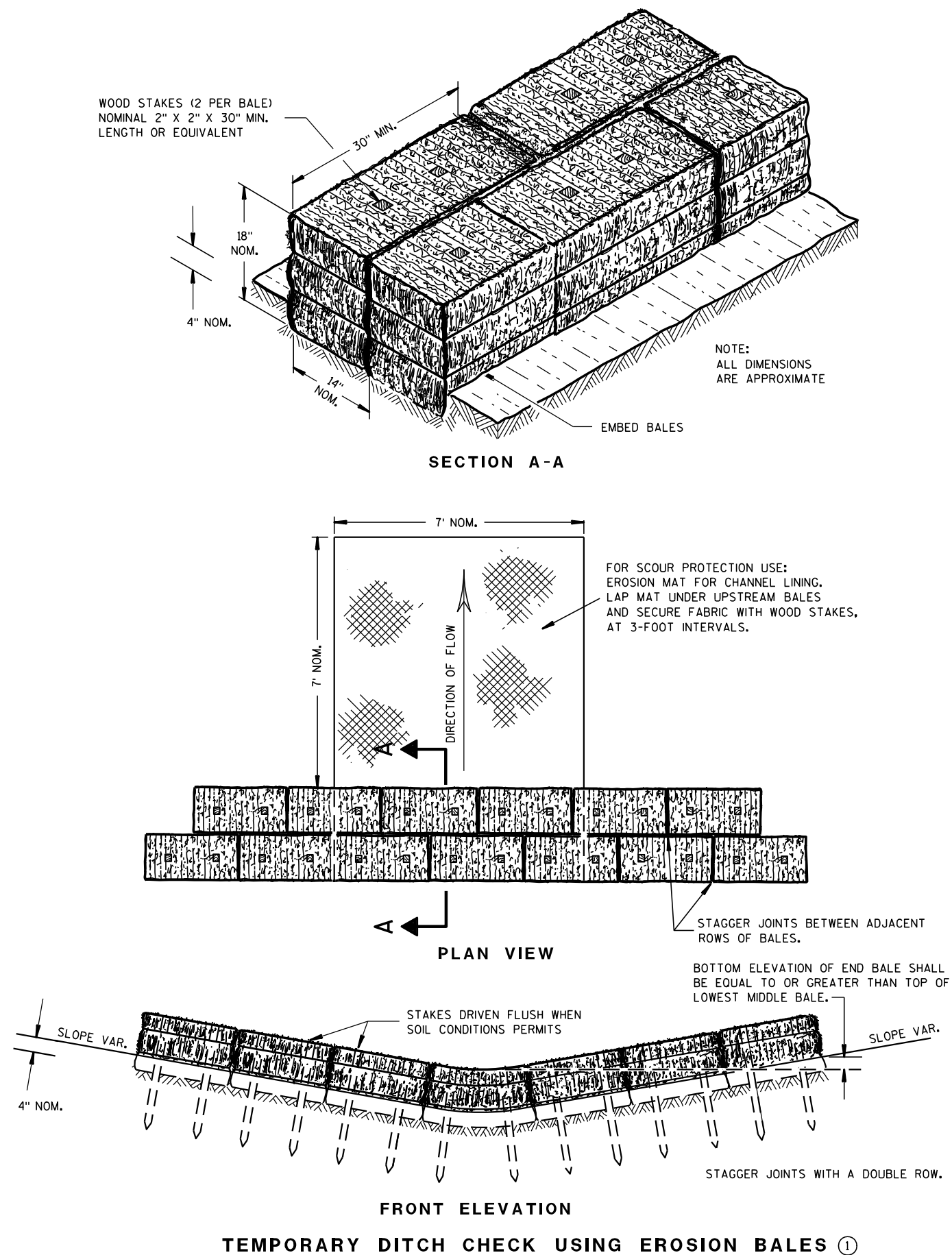
PROJECT NO: 5318-00-72	HWY: PATTERSON ROAD	COUNTY: GREEN	PLAN & PROFILE - PATTERSON ROAD	SHEET	E
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Standard Detail Drawing List

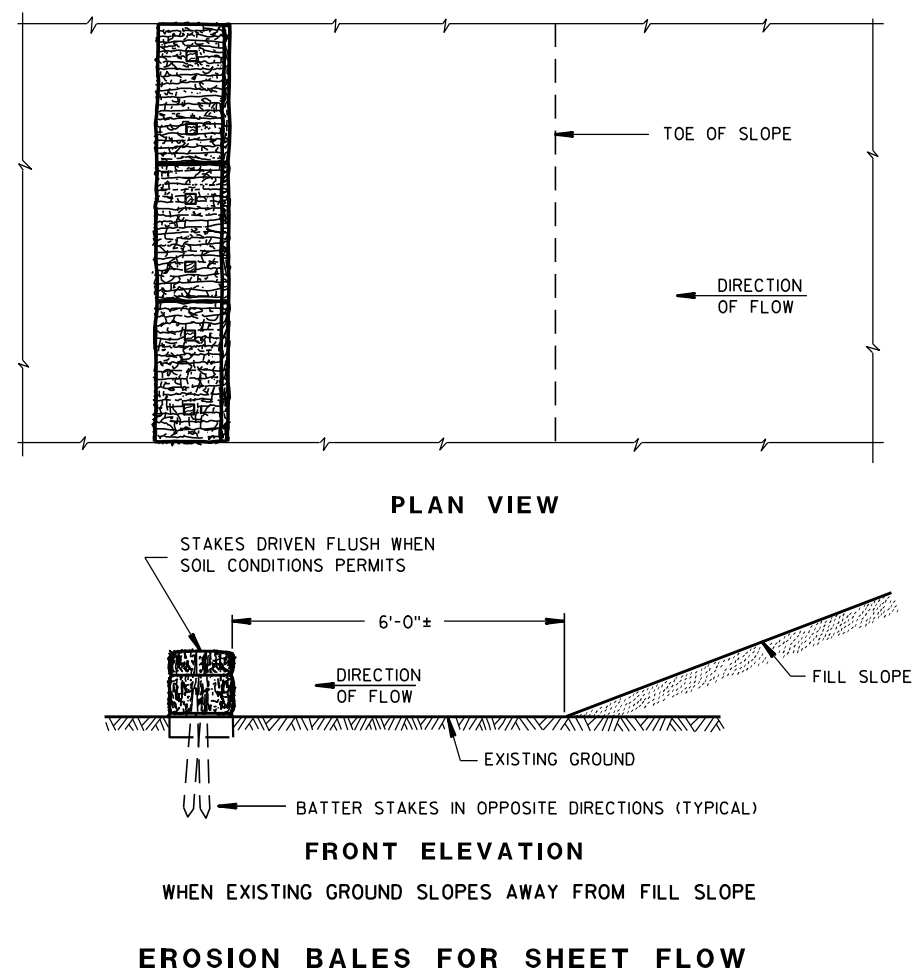
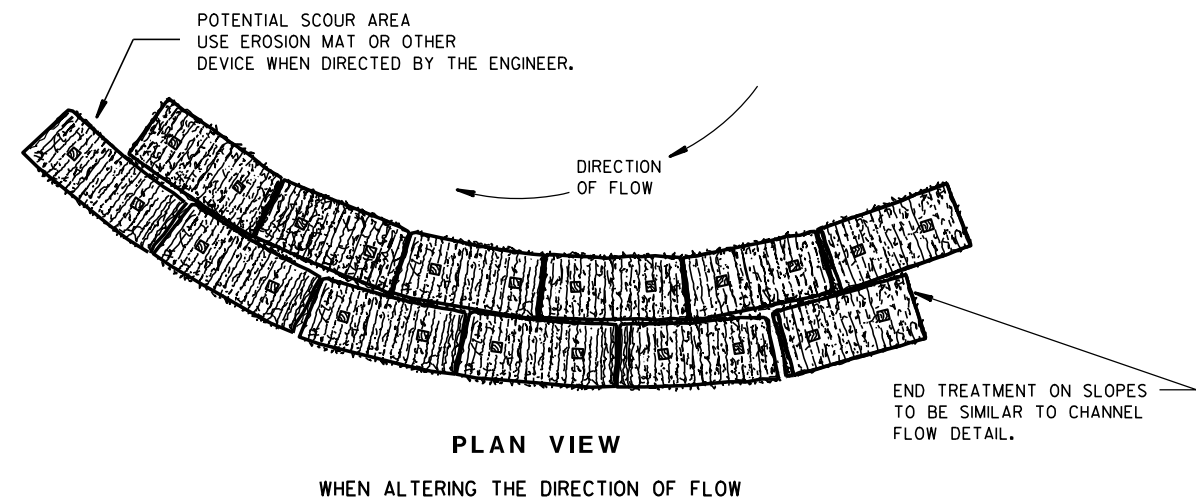
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
15C02-05A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)



## GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

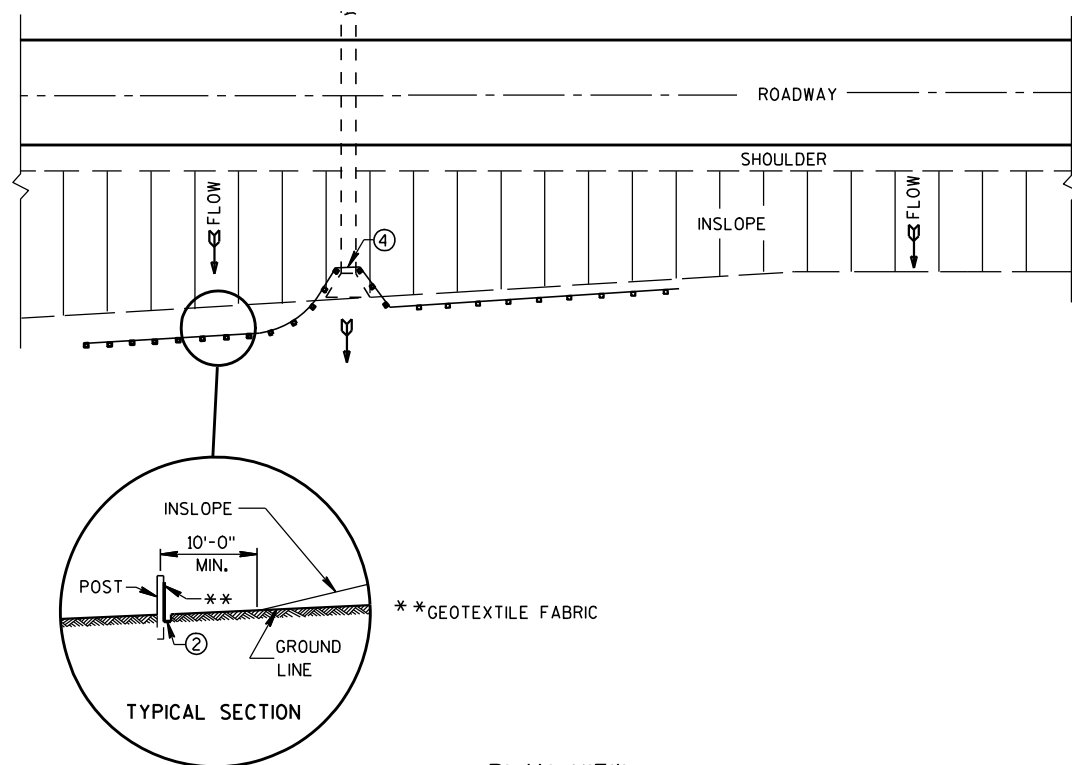
TYPICAL INSTALLATIONS OF  
EROSION BALES / TEMPORARY  
DITCH CHECKS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

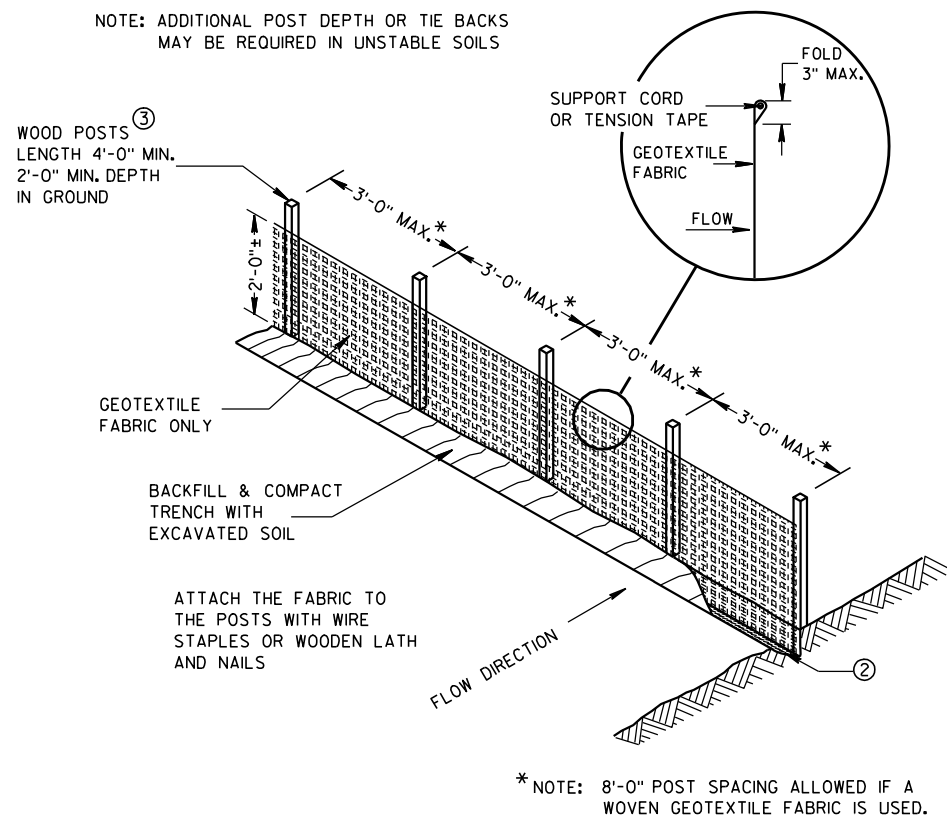
6/04/02  
DATE/S/ Beth Canestra  
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

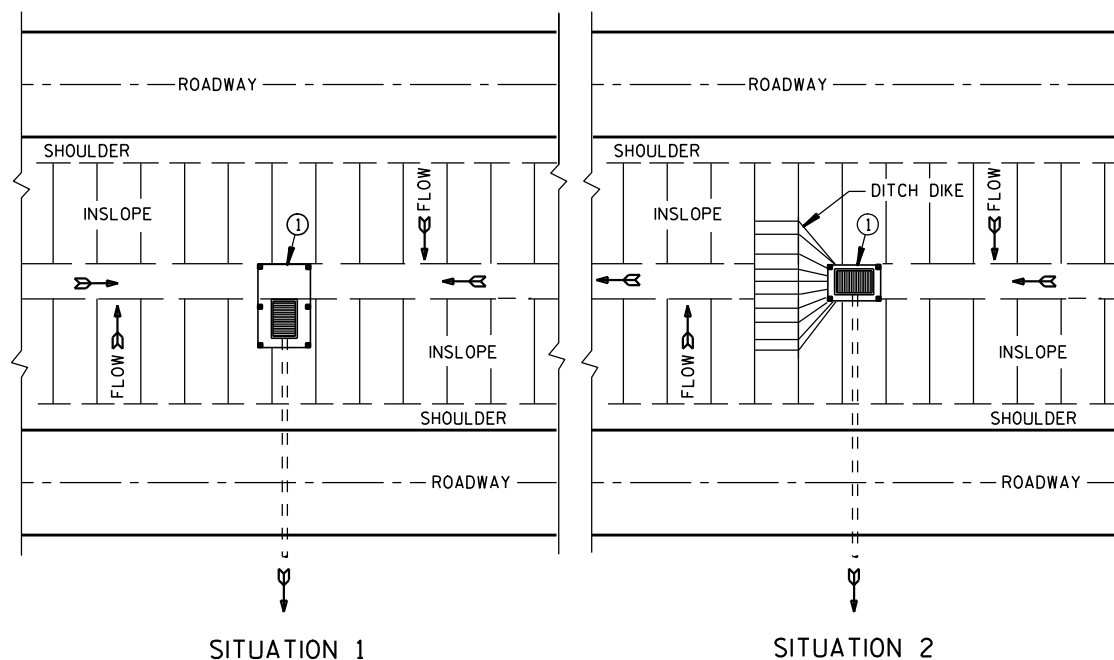


TYPICAL APPLICATION OF SILT FENCE

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

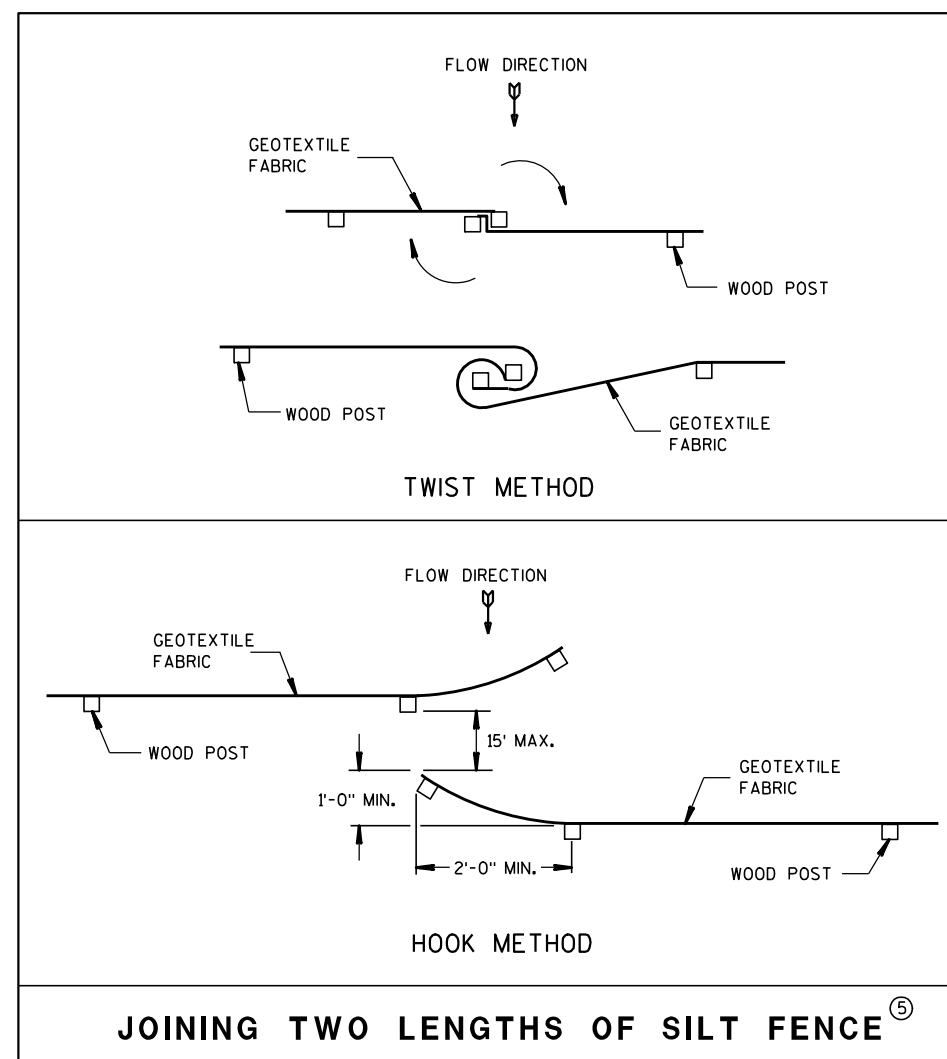


SILT FENCE



PLAN VIEW

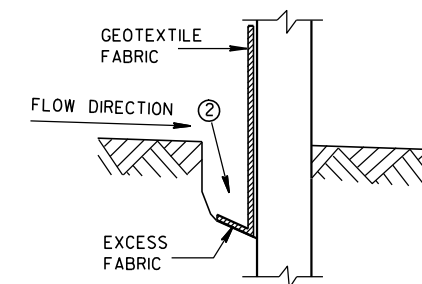
SILT FENCE AT MEDIAN SURFACE DRAINS



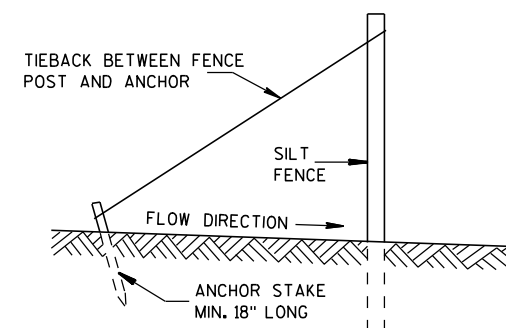
## GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK  
(WHEN REQUIRED BY THE ENGINEER)

## SILT FENCE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED

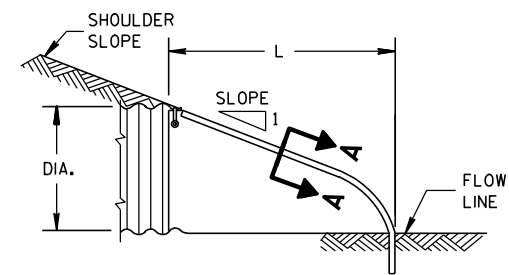
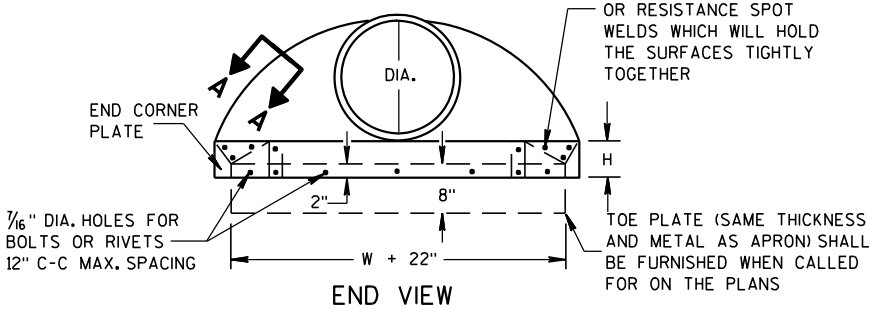
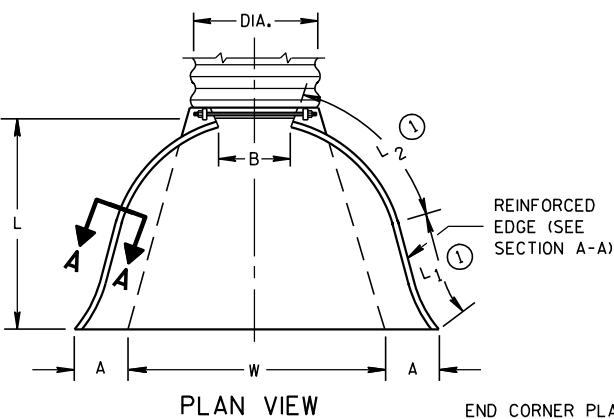
4-29-05  
DATE

FHWA

/S/ Beth Cannestra  
CHIEF ROADWAY DEVELOPMENT ENGINEER

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

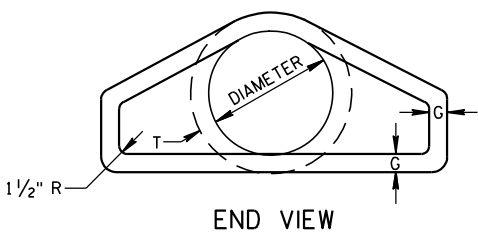
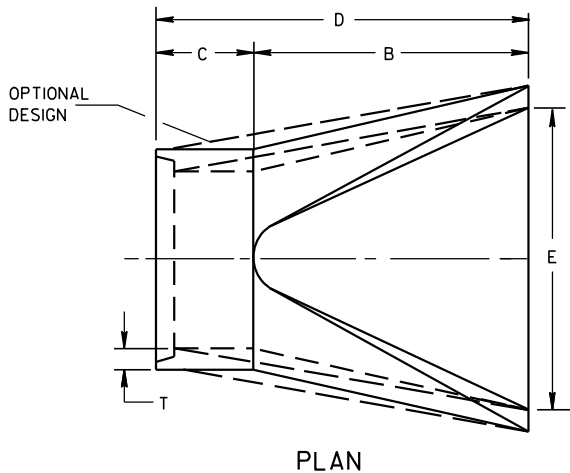
\* EXCEPT CENTER PANEL  
SEE GENERAL NOTES



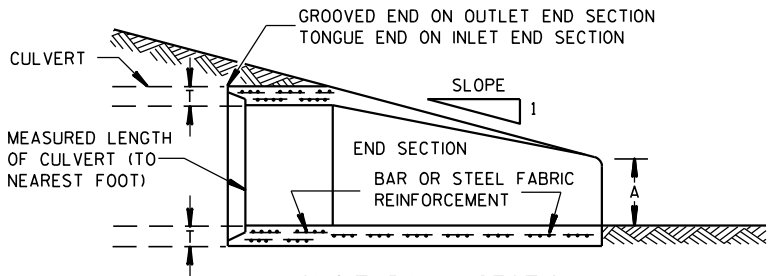
SIDE ELEVATION  
METAL ENDWALLS

REINFORCED CONCRETE APRON ENDWALLS											
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE			
	T	A	B	C	D	E	G				
12	2	4	24	48 7/8	72 1/8	24	2	3 to 1			
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1			
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1			
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1			
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1			
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1			
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1			
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1			
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1			
48	5	24	72	26	98	84	5	3 to 1			
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 2/5 to 1			
60	6	30-35	60	39	99	96	5	2 to 1			
66	6 1/2	24-30	72-78	21-27	99	102	5 1/2	2 to 1			
72	7	24-36	78	21	99	108	6	2 to 1			
78	7 1/2	24-36	78	21	99	114	6 1/2	2 to 1			
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1			
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1			

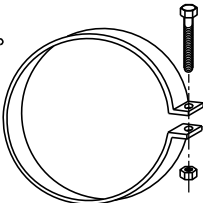
\* MINIMUM  
\*\* MAXIMUM



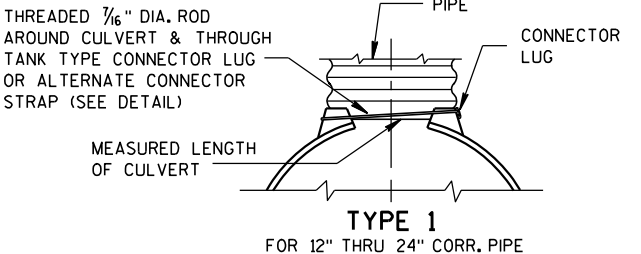
LONGITUDINAL SECTION  
CONCRETE ENDWALLS



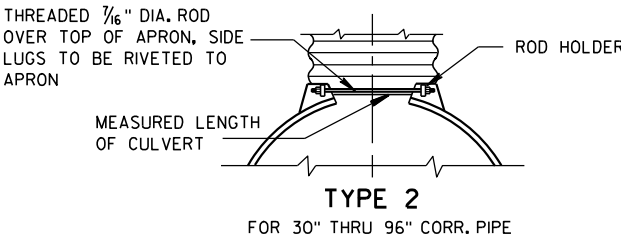
1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



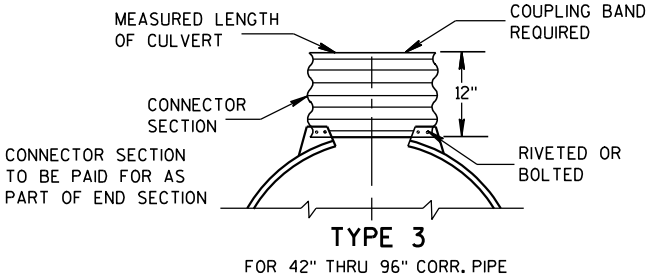
ALTERNATE FOR TYPE 1 CONNECTION  
END SECTION CONNECTOR STRAP



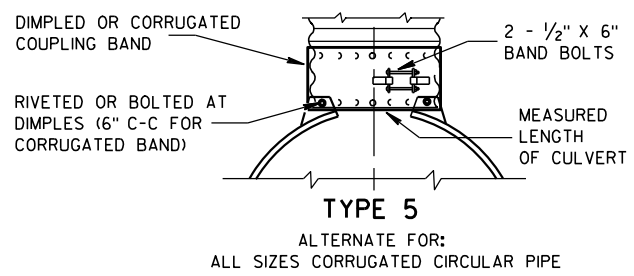
TYPE 1  
FOR 12" THRU 24" CORR. PIPE



TYPE 2  
FOR 30" THRU 96" CORR. PIPE



TYPE 3  
FOR 42" THRU 96" CORR. PIPE



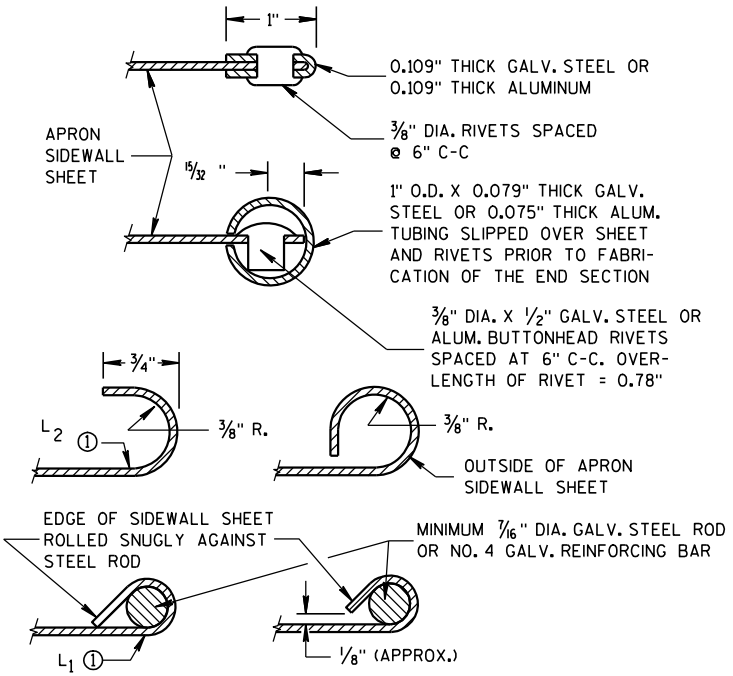
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR  
CULVERT PIPE

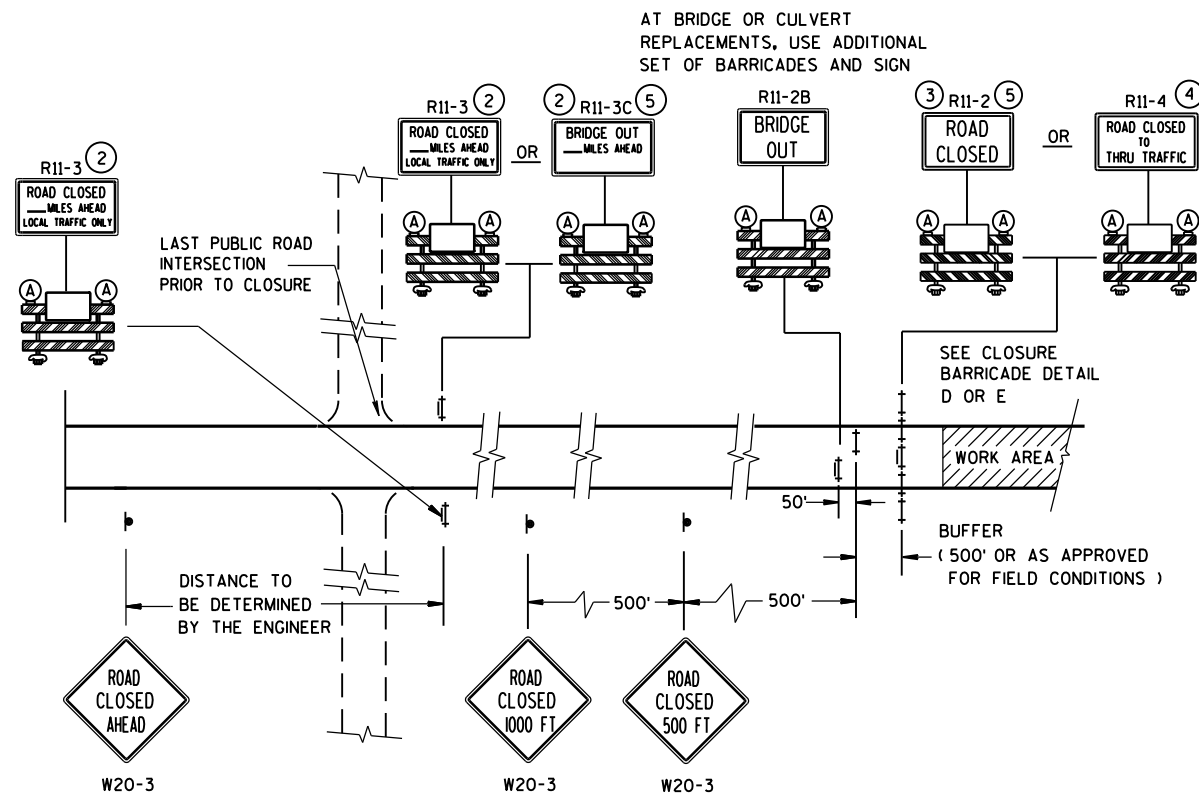
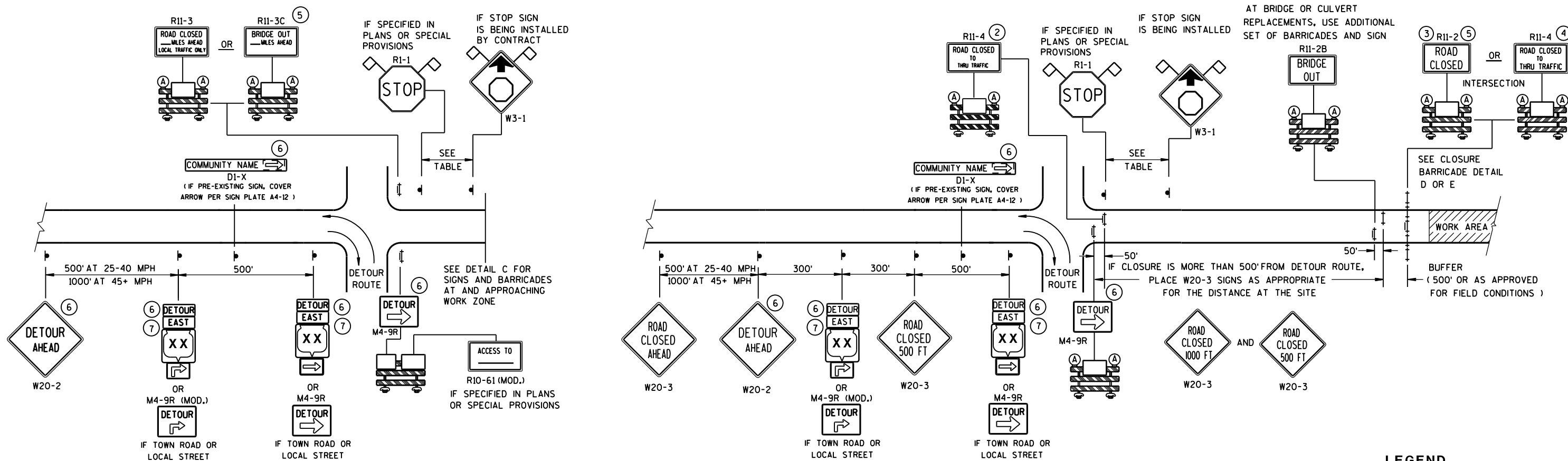
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
11/30/94  
DATE

/S/ Rory L. Rhinesmith  
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA



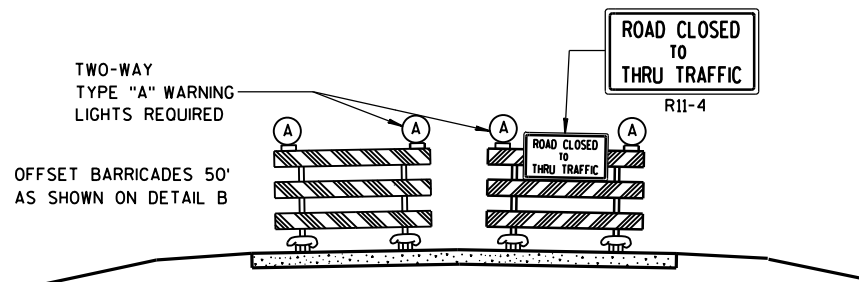


SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦

<b>BARRICADES AND SIGNS FOR MAINLINE CLOSURES</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



**DETAIL D**  
**ROAD CLOSURE BARRICADE DETAIL**  
APPROACH VIEW



**DETAIL E**  
**LANE CLOSURE BARRICADE DETAIL**  
APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

### GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".

M4-9 SHALL BE 30" X 24".

M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)

M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)

M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)

M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)

D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

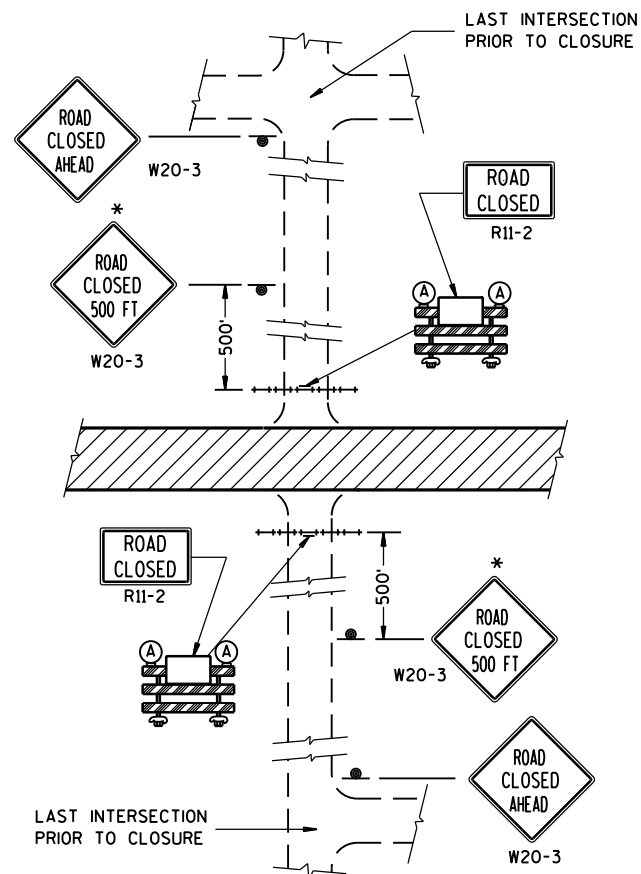
R1-1 SHALL BE 36" X 36".

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

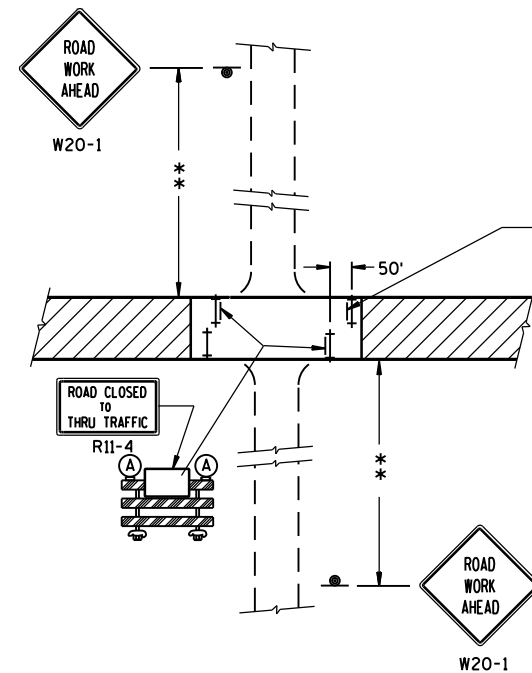
**BARRICADES AND SIGNS  
FOR  
MAINLINE CLOSURES**

**STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION**

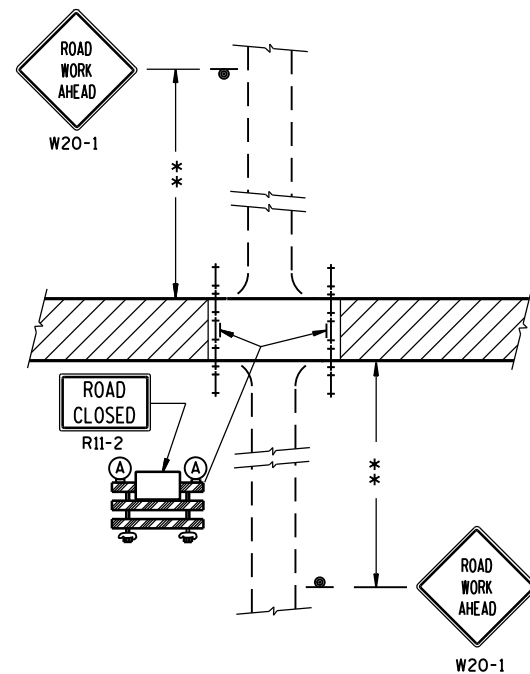
8/2013 /S/ Travis Feltes  
DATE STATE TRAFFIC ENGINEER OF DESIGN  
FHWA



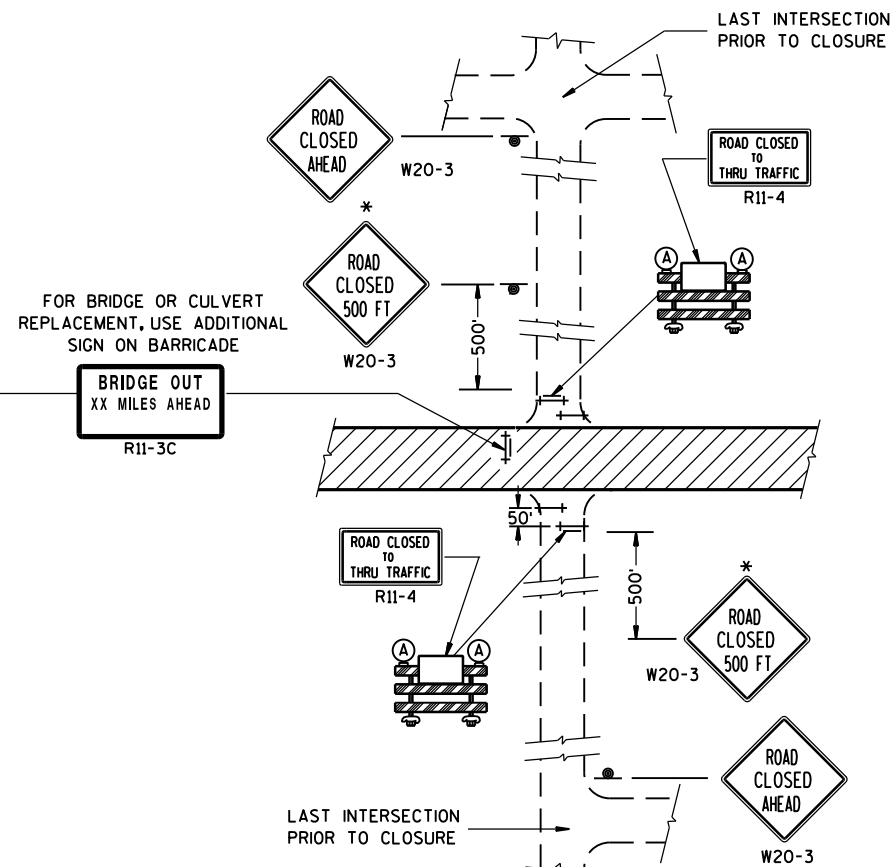
**DETAIL 1**  
(NO ACCESS TO PROJECT)



**DETAIL 3**  
(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



**DETAIL 2**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
NO ACCESS TO PROJECT).



**DETAIL 4**  
(CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)

## GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-4 AND R11-3 SHALL BE 60" X 30".

\*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

\*\*500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

## LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

## BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION





APPROVED

8/2013 /S/ Travis Feltes

DATE STATE TRAFFIC ENGINEER OF DESIGN

FHWA

LEGEND

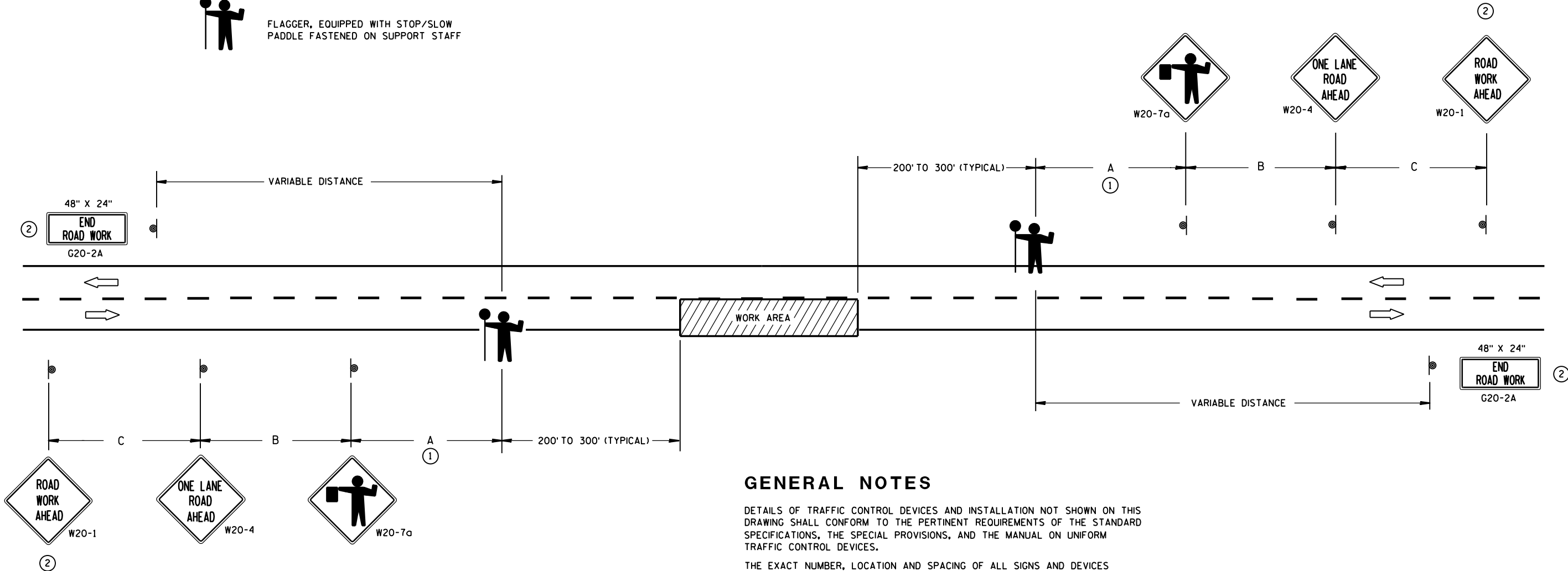
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

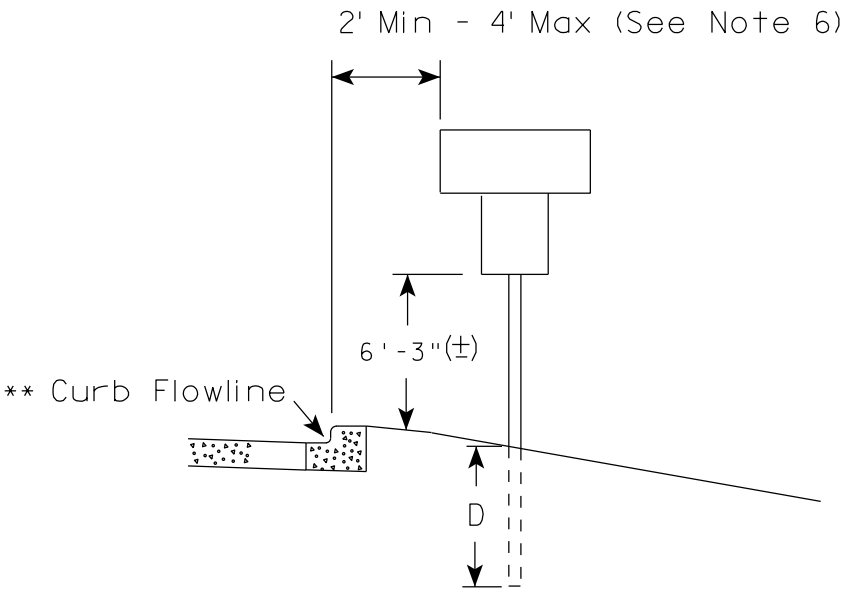
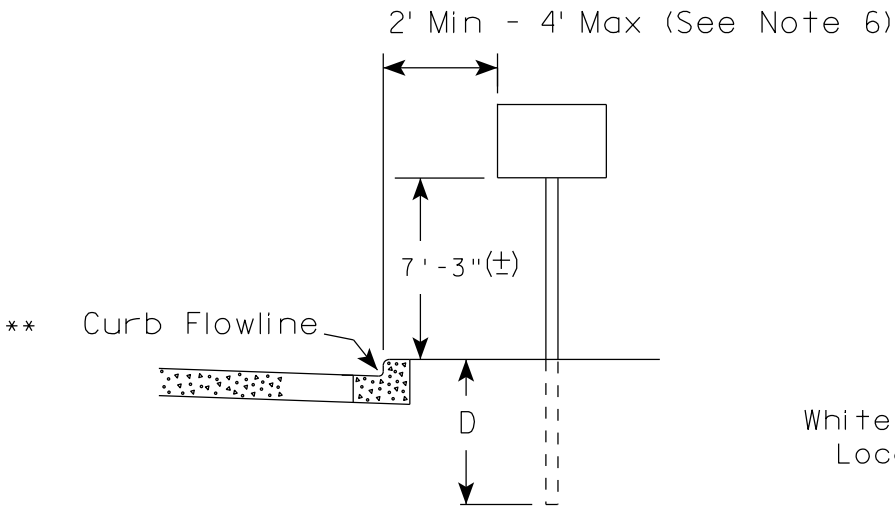
- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

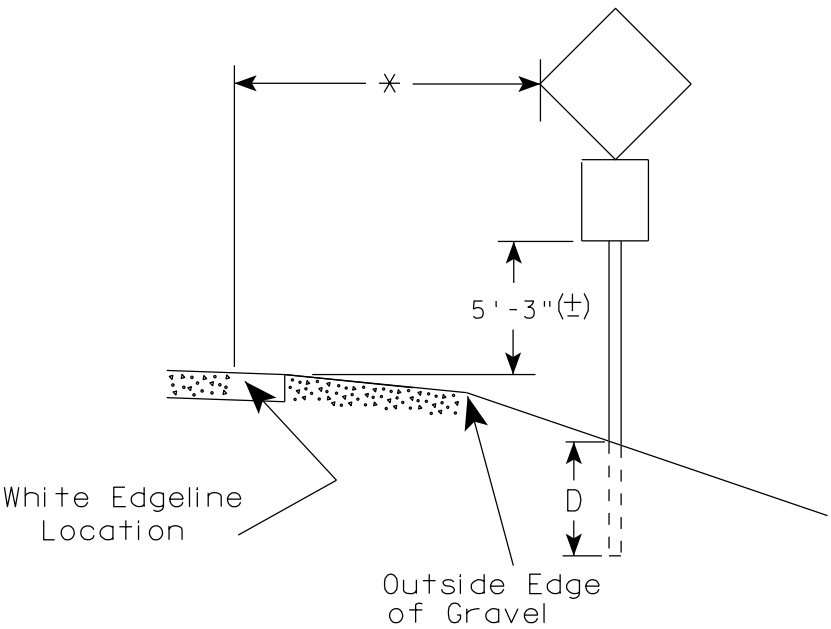
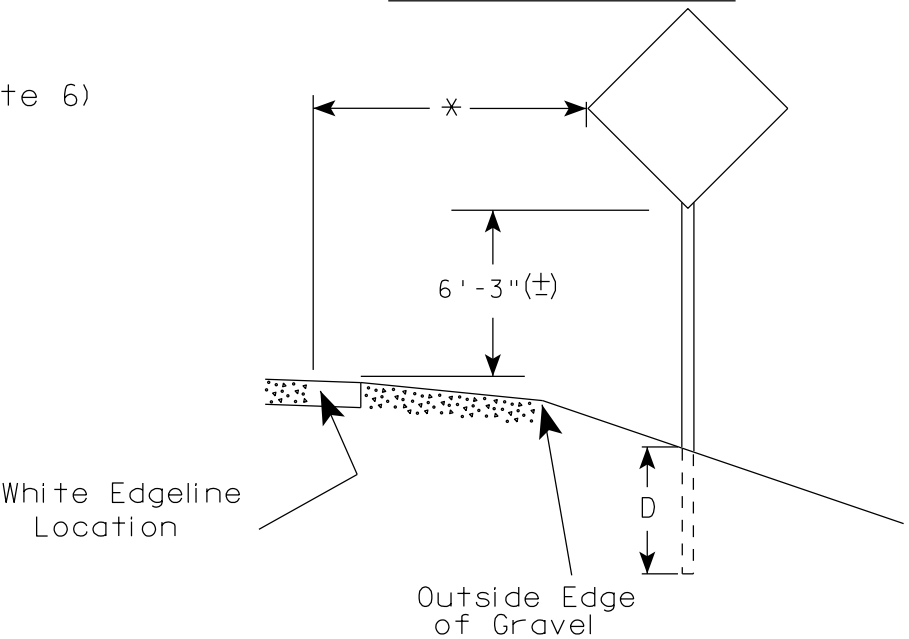
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
8/2013 /S/ Travis Feltes  
DATE STATE TRAFFIC ENGINEER OF DESIGN  
FHWA

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on barrier wall, see A4-10 sign plate.
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for J assemblies (A4-5) is 7'-3" (±) or 6'-3" (±) per urban or rural detail respectively.
5. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. The (±) tolerance for mounting height is 3 inches.
8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.
9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series) & End of Rod Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (+).

POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq.Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

×× The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

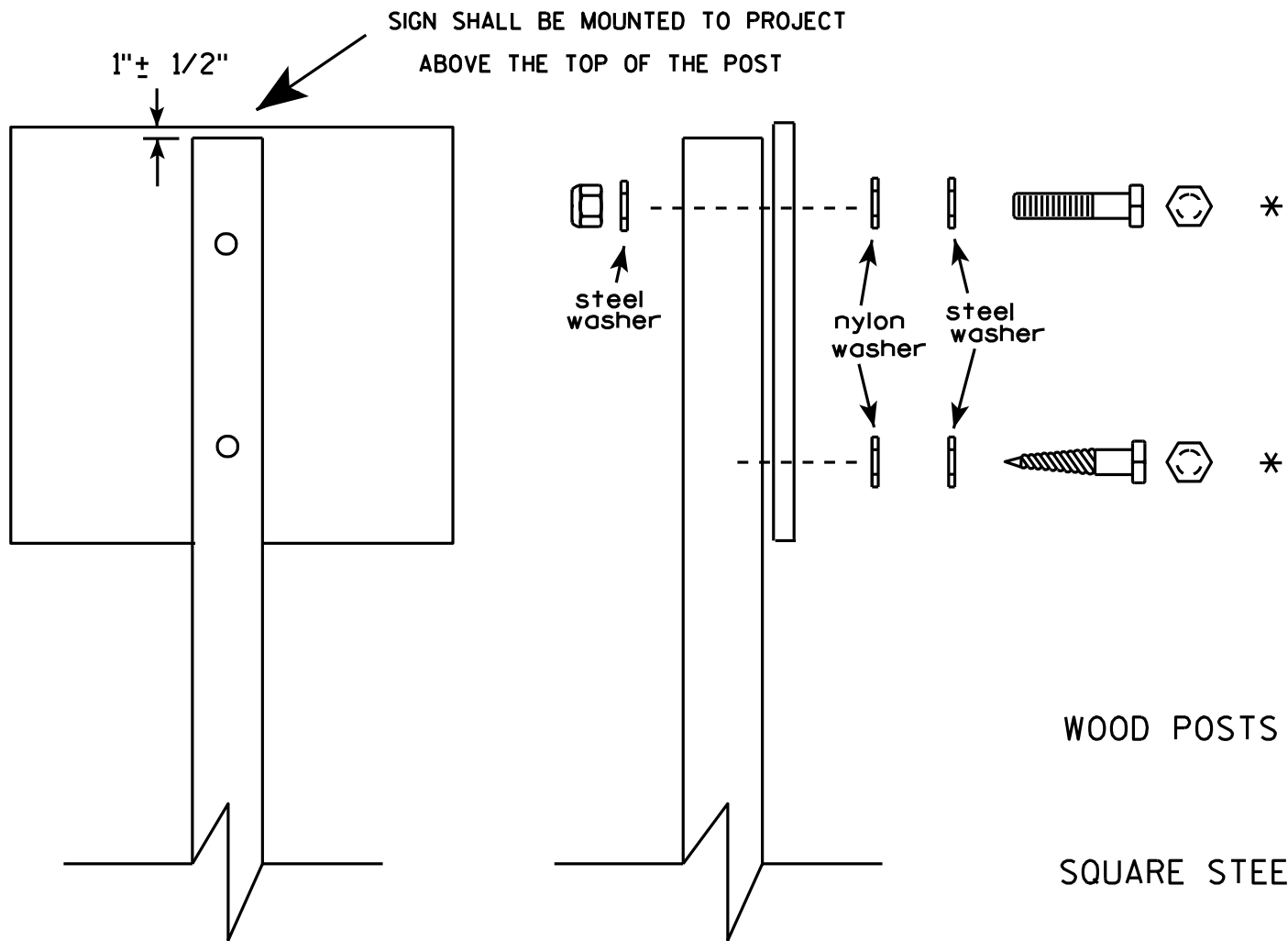
\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION  
OF PERMANENT TYPE II  
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 9/30/13 PLATE NO. A4-3.18

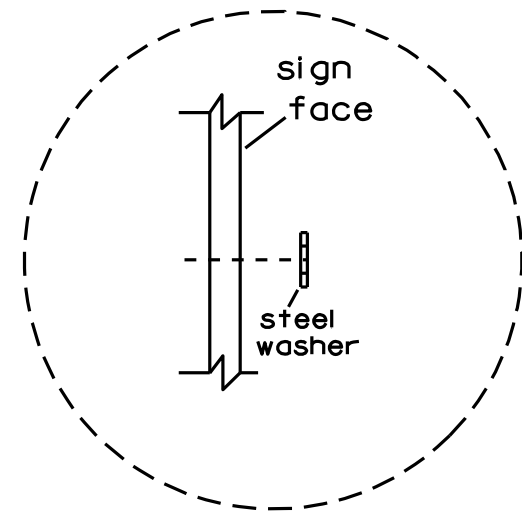


Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

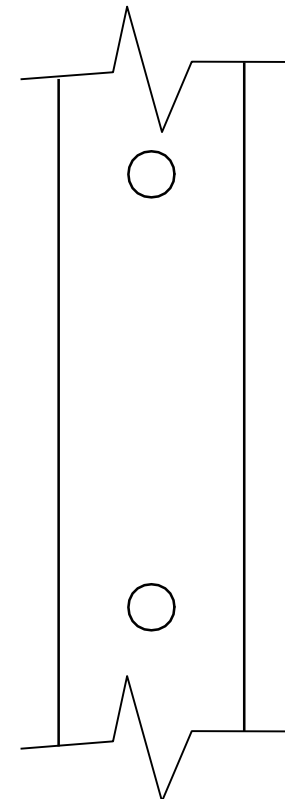
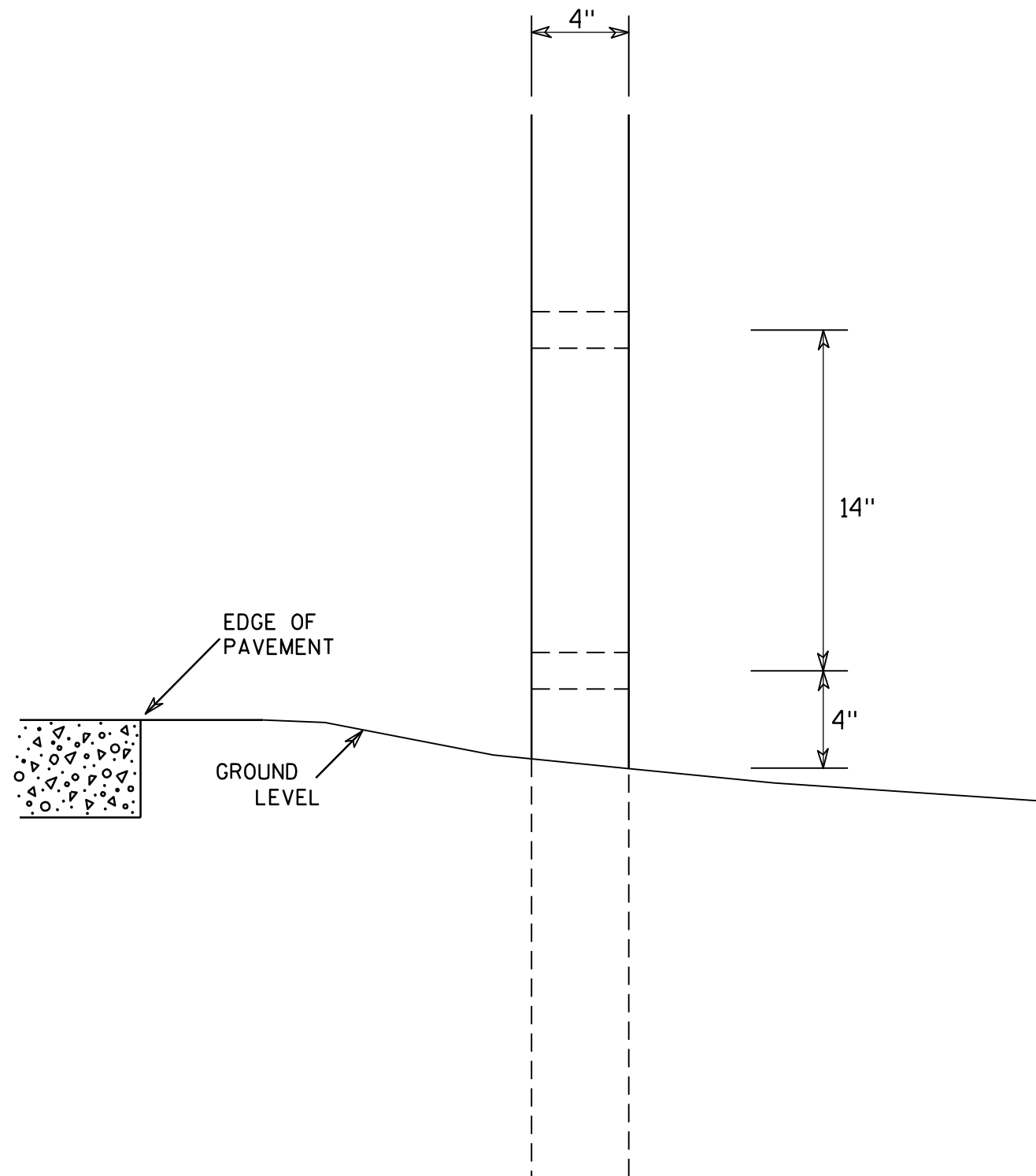
- WOOD POSTS (4" x 4" or 4" x 6")  
LAG SCREWS - 3/8" X 3"  
MACHINE BOLTS - 5/16" X 6-1/2" or 7" Length w/ nuts
- SQUARE STEEL POSTS (2" x 2")  
MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts  
RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL  
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -  
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL  
1-1/4" O.D. X 3/8" I.D. X .080 NYLON for all Type H signs.



Washer Placement when Sign Has Other Than Type H or Type F Face

\* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 3/23/10	PLATE NO. A4-8.7



SIDE VIEW

# GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

## 4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Chester J. Spang*  
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO: 5318-00-72

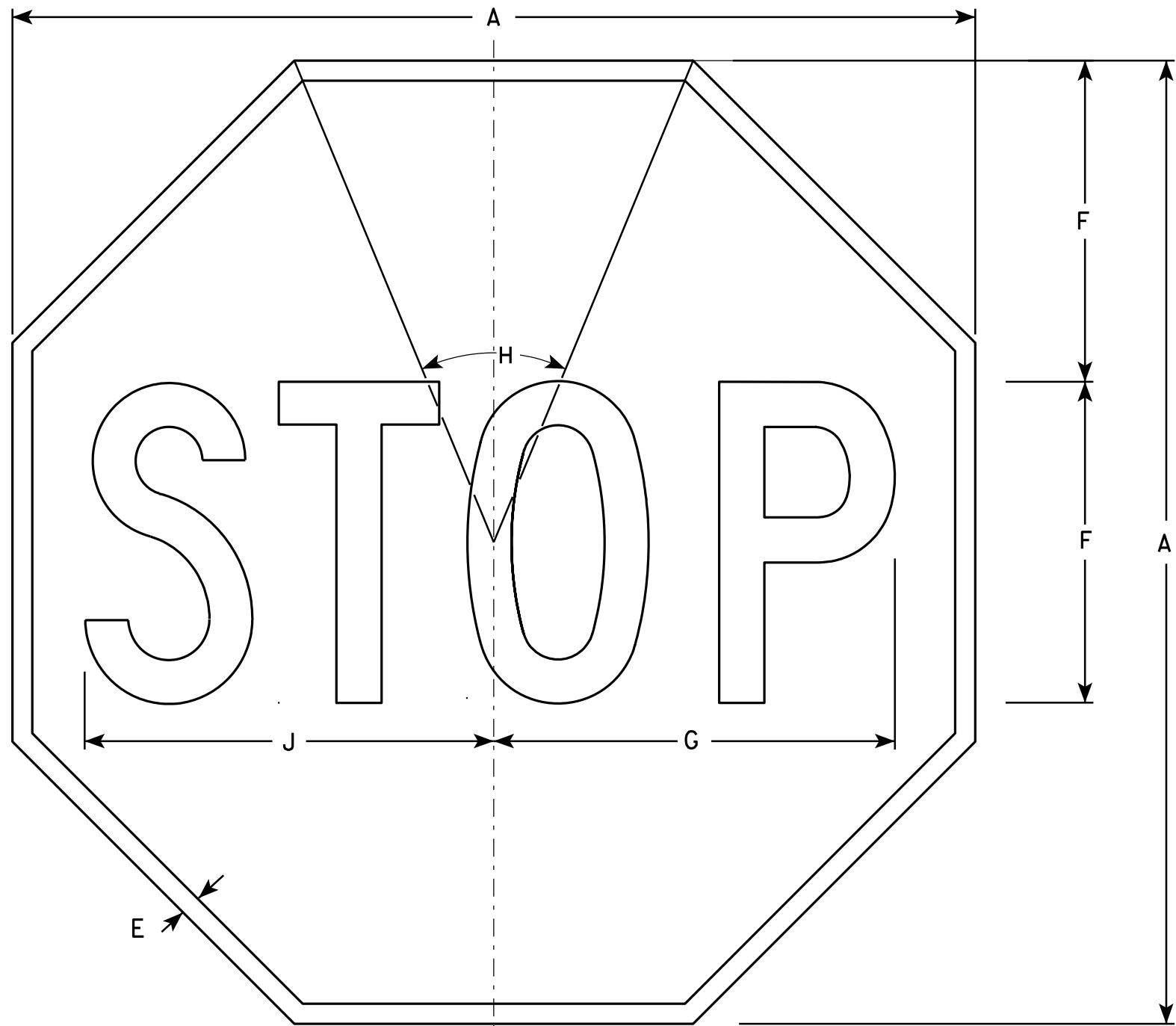
HWY: PATTERSON ROAD

COUNTY: GREEN

SIGN DETAILS

SHEET NO:

E



**NOTES**

- 1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:  
Background - Red  
Message - White
- 3. Message Series - C

R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24				3/8	8	10	45°		10 1/4																	3.31
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN  
R1 - 1

WISCONSIN DEPT OF TRANSPORTATION

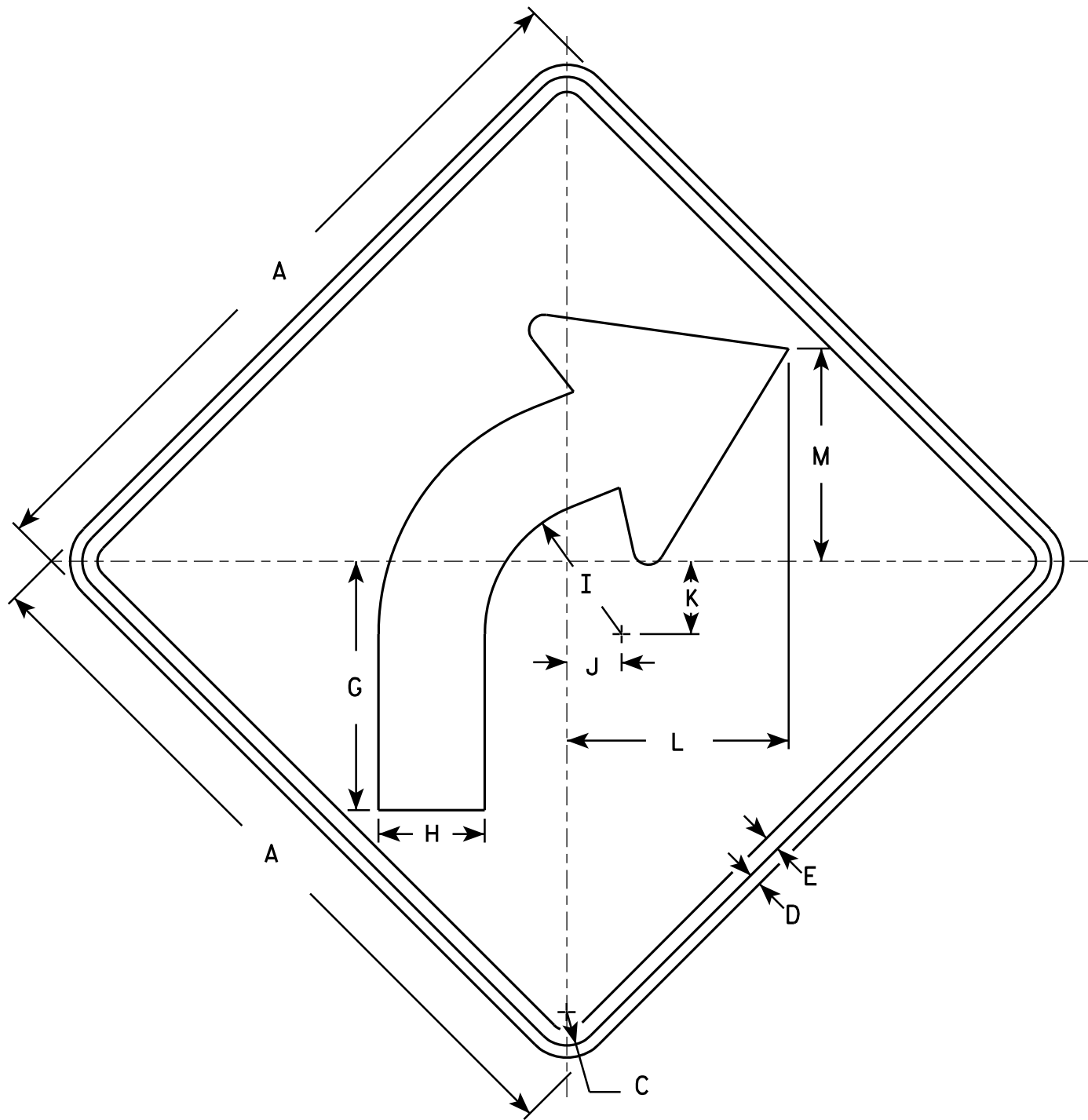
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-1.12

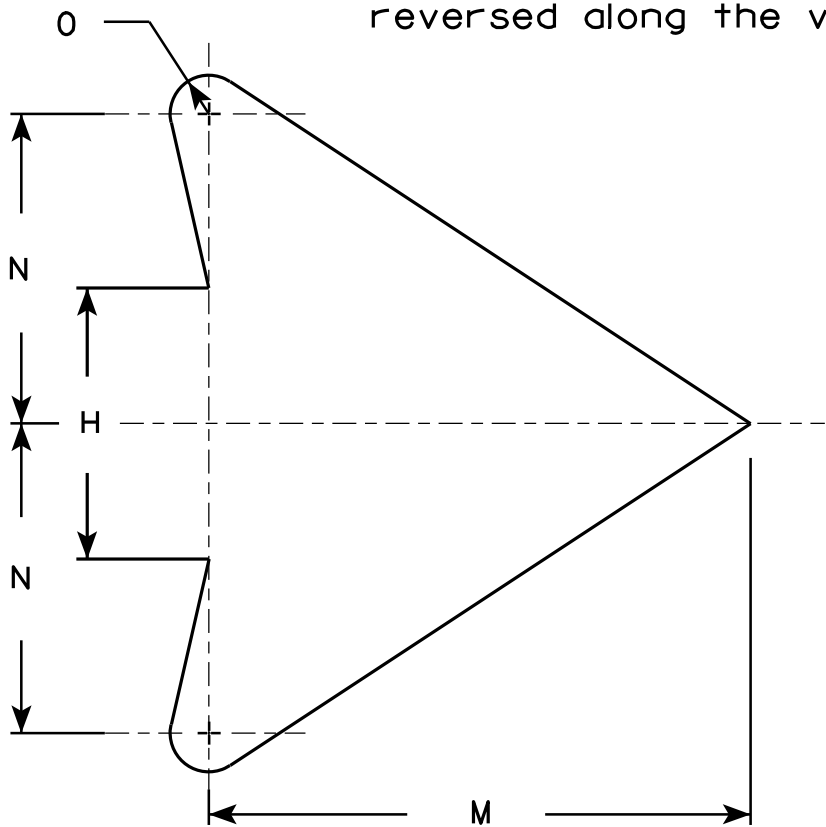


NOTES

1. Sign is Type II - Type F Reflective - reference  
WIS DOT Standard Specification for HIGHWAY  
and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base  
material is plywood but borders shall be rounded  
as shown. When base material is metal, the  
corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is  
reversed along the vertical centerline.



W1-2R



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

STANDARD SIGN  
W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED  
*Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10

PROJECT NO: 5318-00-72

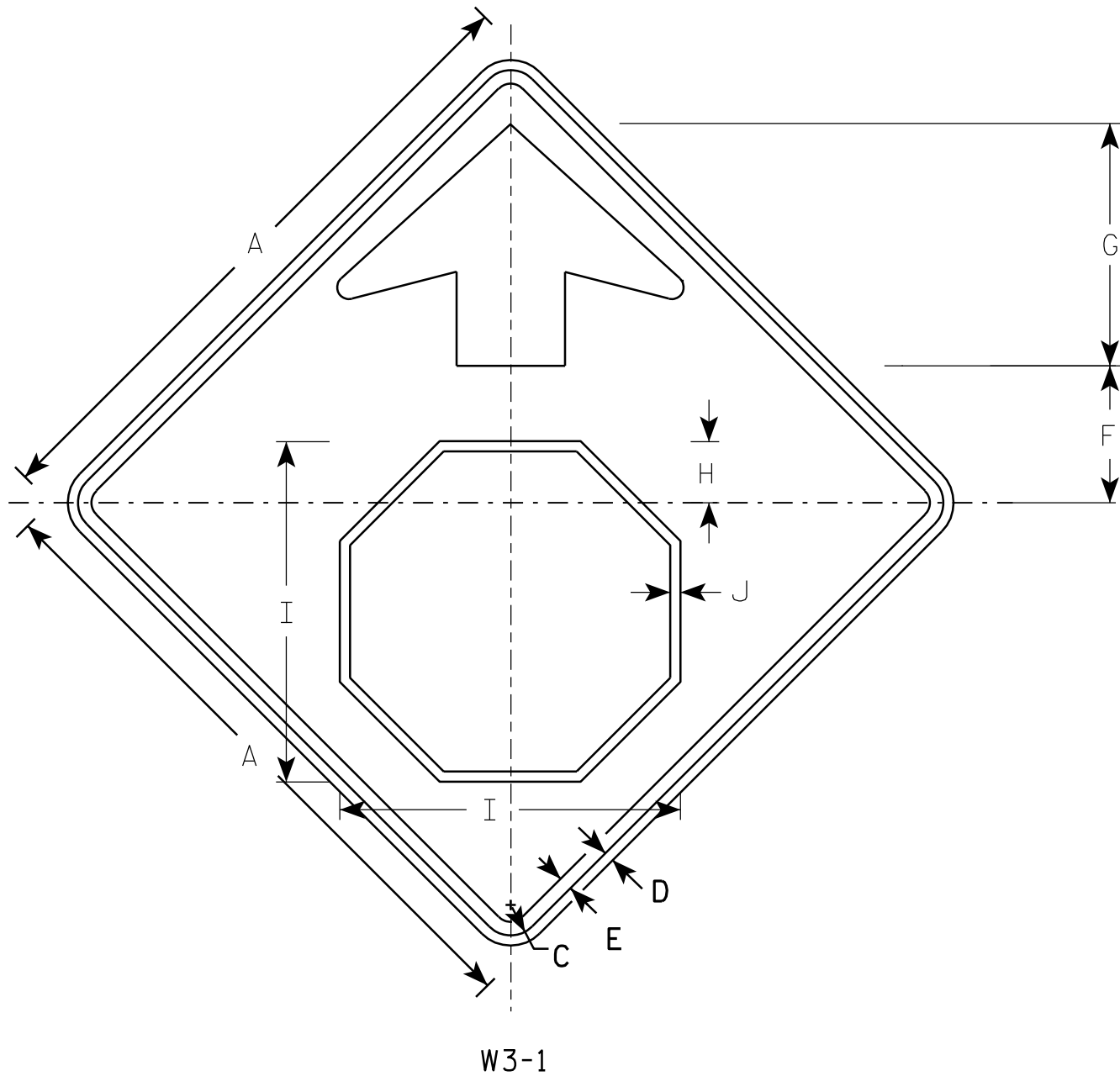
HWY: PATTERSON ROAD

COUNTY: GREEN

SIGN PLATES

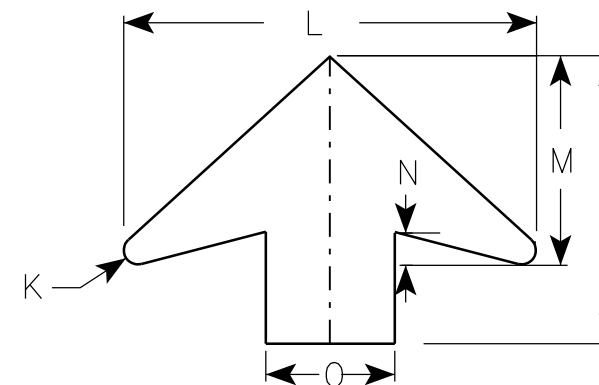
SHEET NO:

E



### NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - YELLOW  
Arrow & Border - BLACK  
Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 <sup>3</sup> / <sub>8</sub>	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

PROJECT NO: 5318-00-72

HWY: PATTERSON ROAD

COUNTY: GREEN

SIGN PLATES

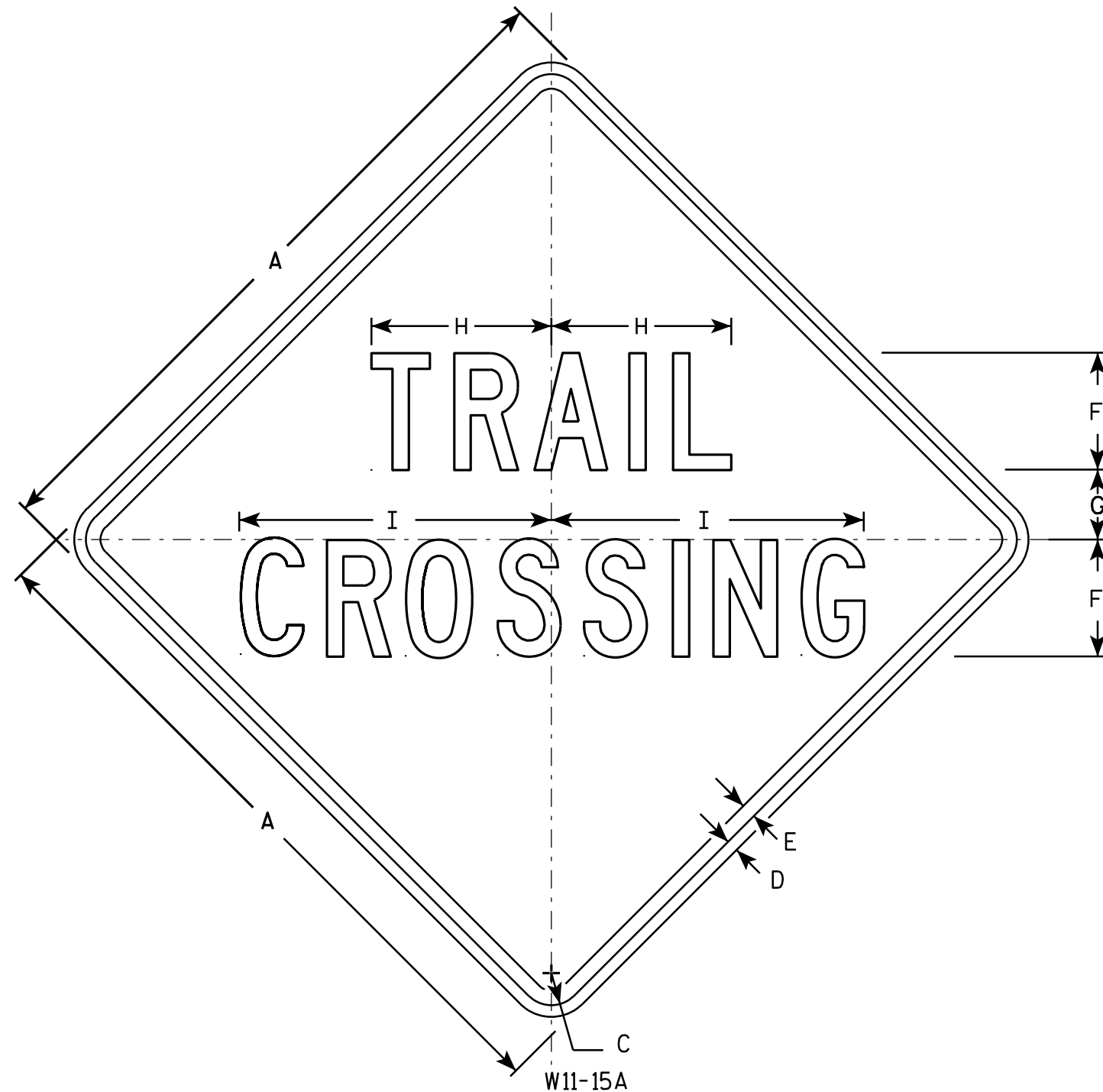
SHEET NO:

E

STANDARD SIGN  
W3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer  
DATE 6/7/10 PLATE NO. W3-1.12



### NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4	2 3/8	6 1/8	10 3/4																		4.0
2S	30		1 3/8	1/2	5/8	5	3	7 3/4	13 3/8																		6.25
2M	36		1 5/8	5/8	3/4	6	3 1/2	9 1/4	16																		9.0
3	36		1 5/8	5/8	3/4	6	3 1/2	9 1/4	16																		9.0
4	48		2 1/4	3/4	1	8	5	12 7/8	21 1/4																		16.0
5																											

### STANDARD SIGN W11-15A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 12/21/10 PLATE NO. W11-15A.1

PROJECT NO: 5318-00-72

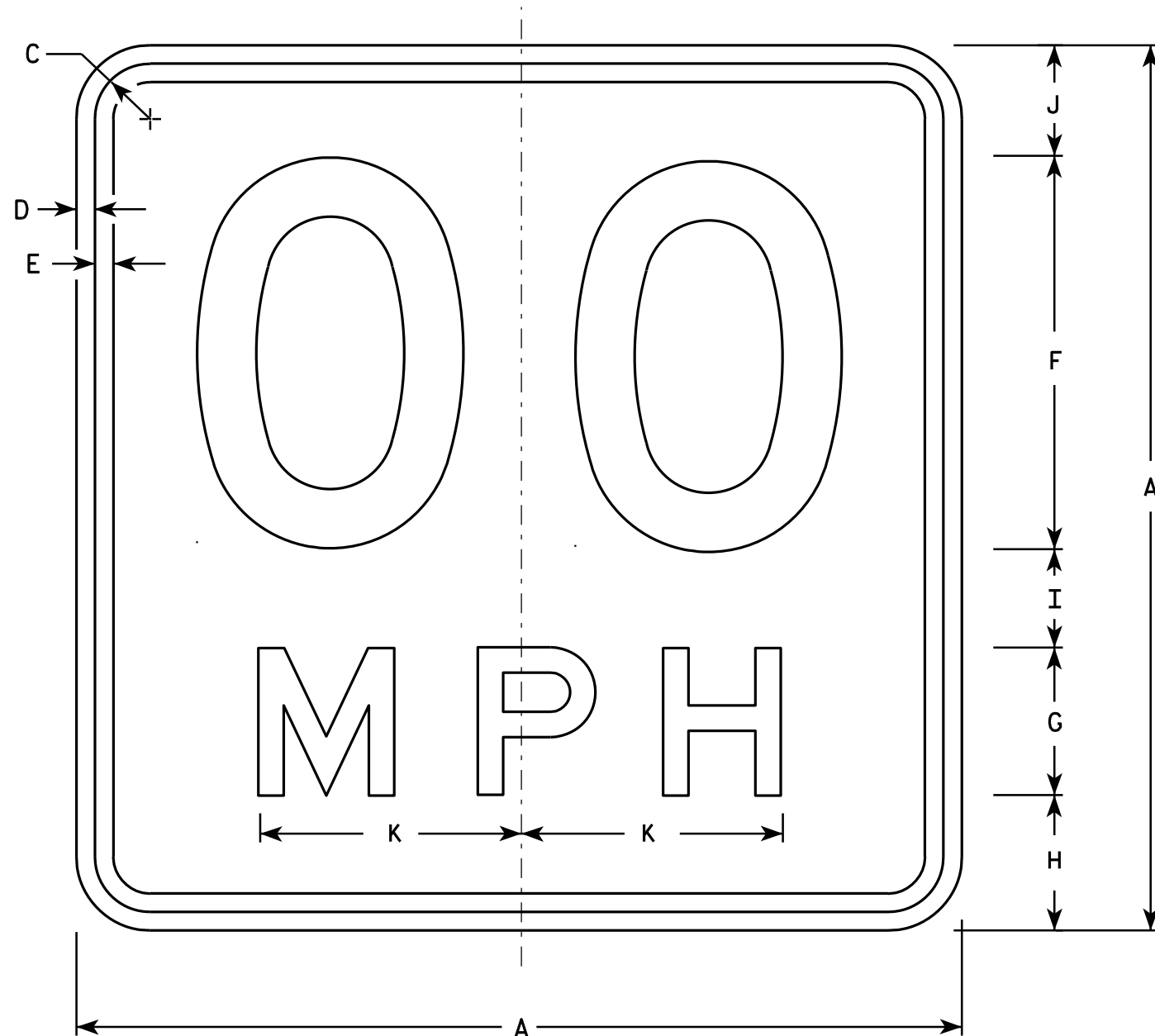
HWY: PATTERSON ROAD

COUNTY: GREEN

SIGNS PLATES

SHEET NO:

E



### NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D  
Line 2 is Series E

W13-1

- \* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.  
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

### STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

PROJECT NO: 5318-00-72

HWY: PATTERSON ROAD

COUNTY: GREEN

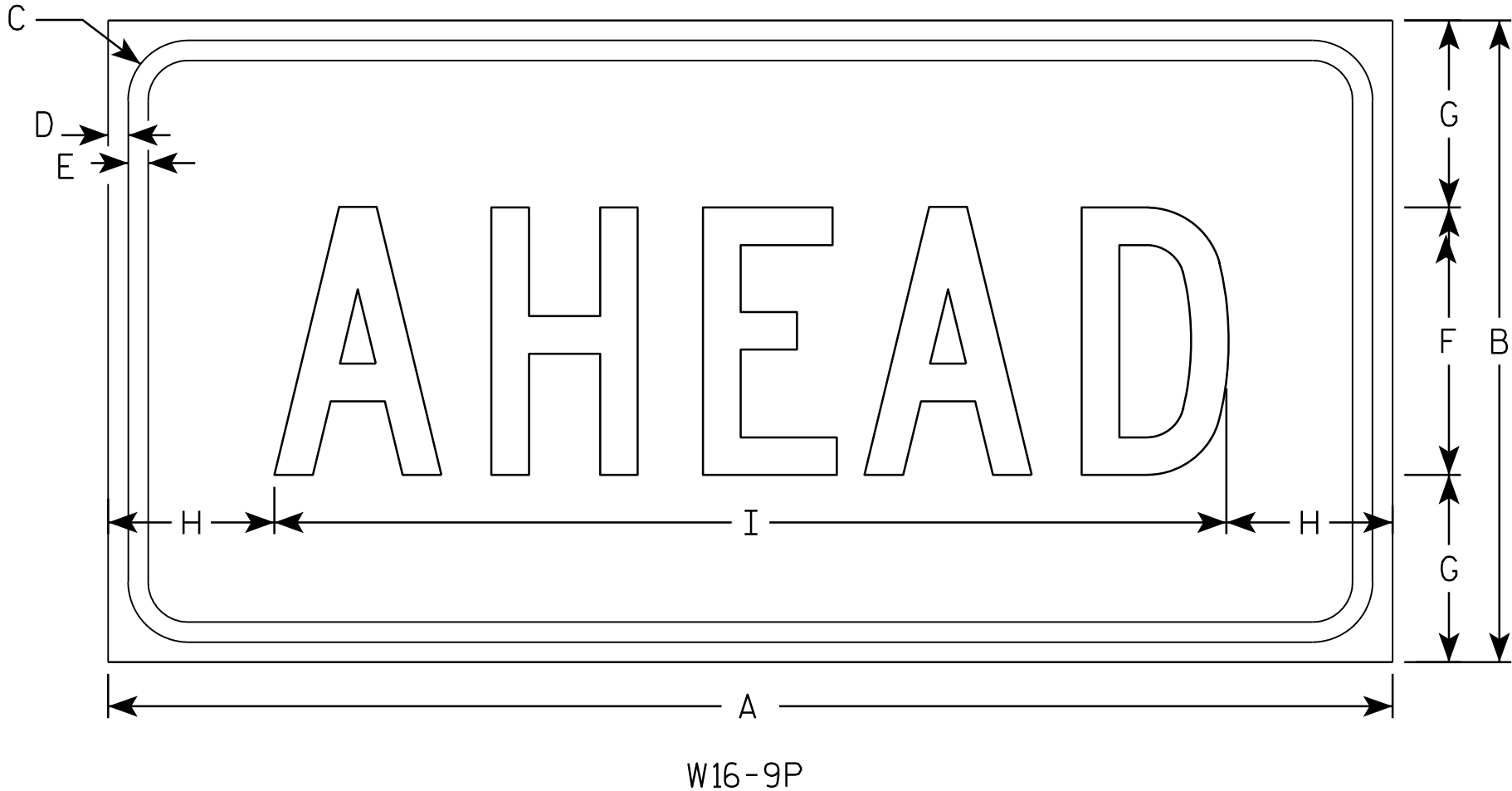
SIGNS PLATES

SHEET NO:

E

NOTES

- 1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:  
Background - Yellow  
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
2M	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/8	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

STANDARD SIGN

W16-9P

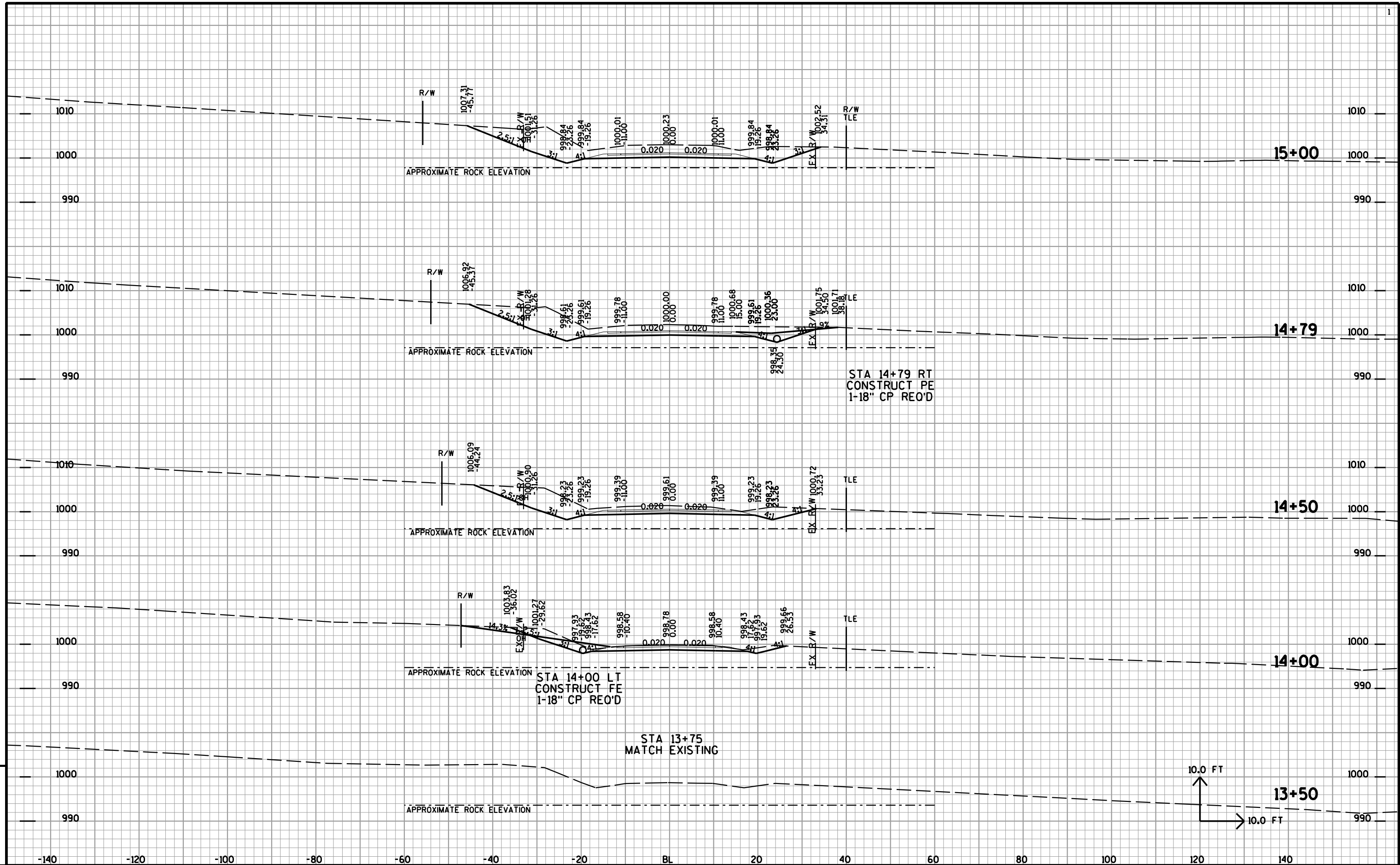
WISCONSIN DEPT OF TRANSPORTATION

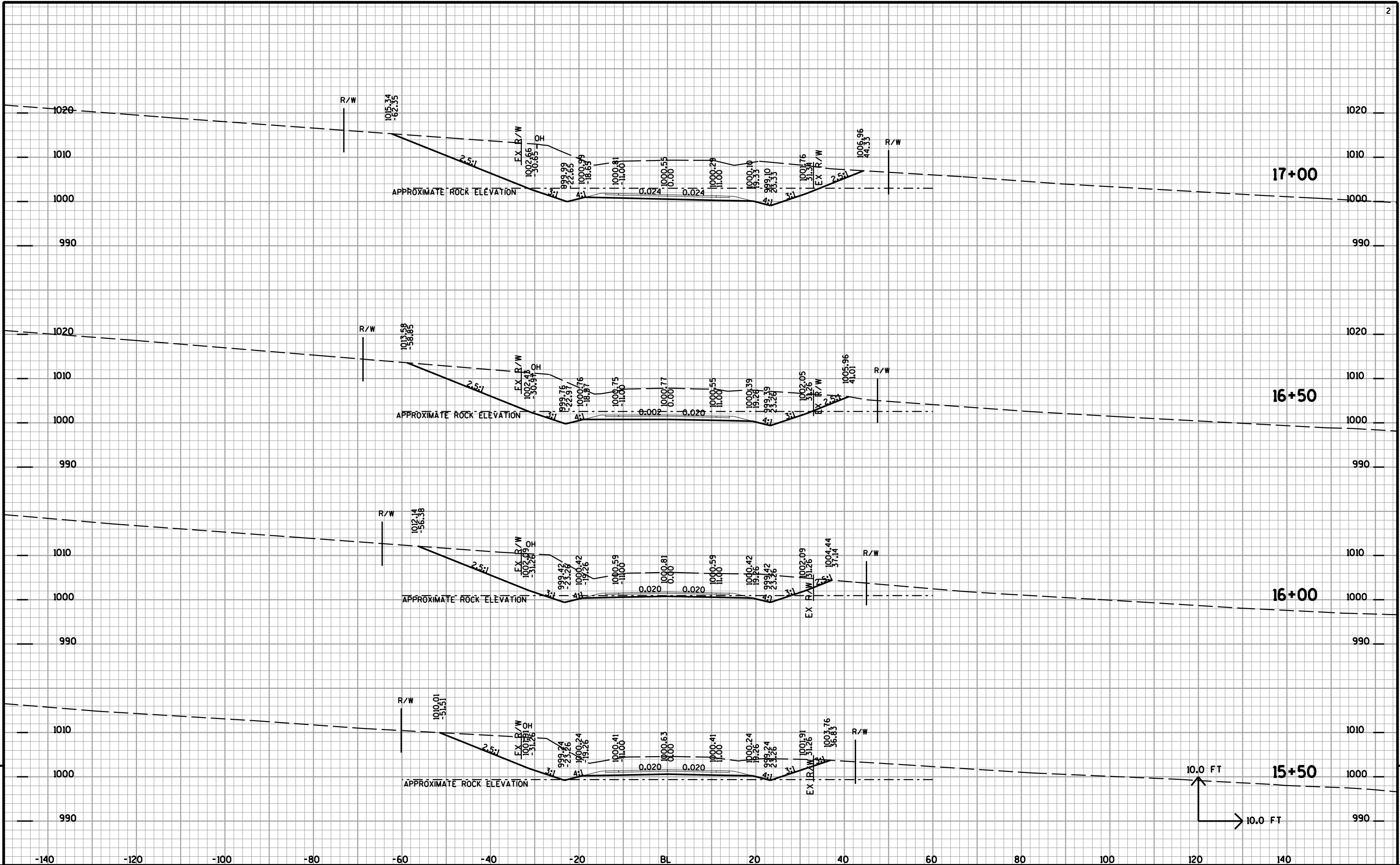
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 12/28/10 PLATE NO. W16-9P.6

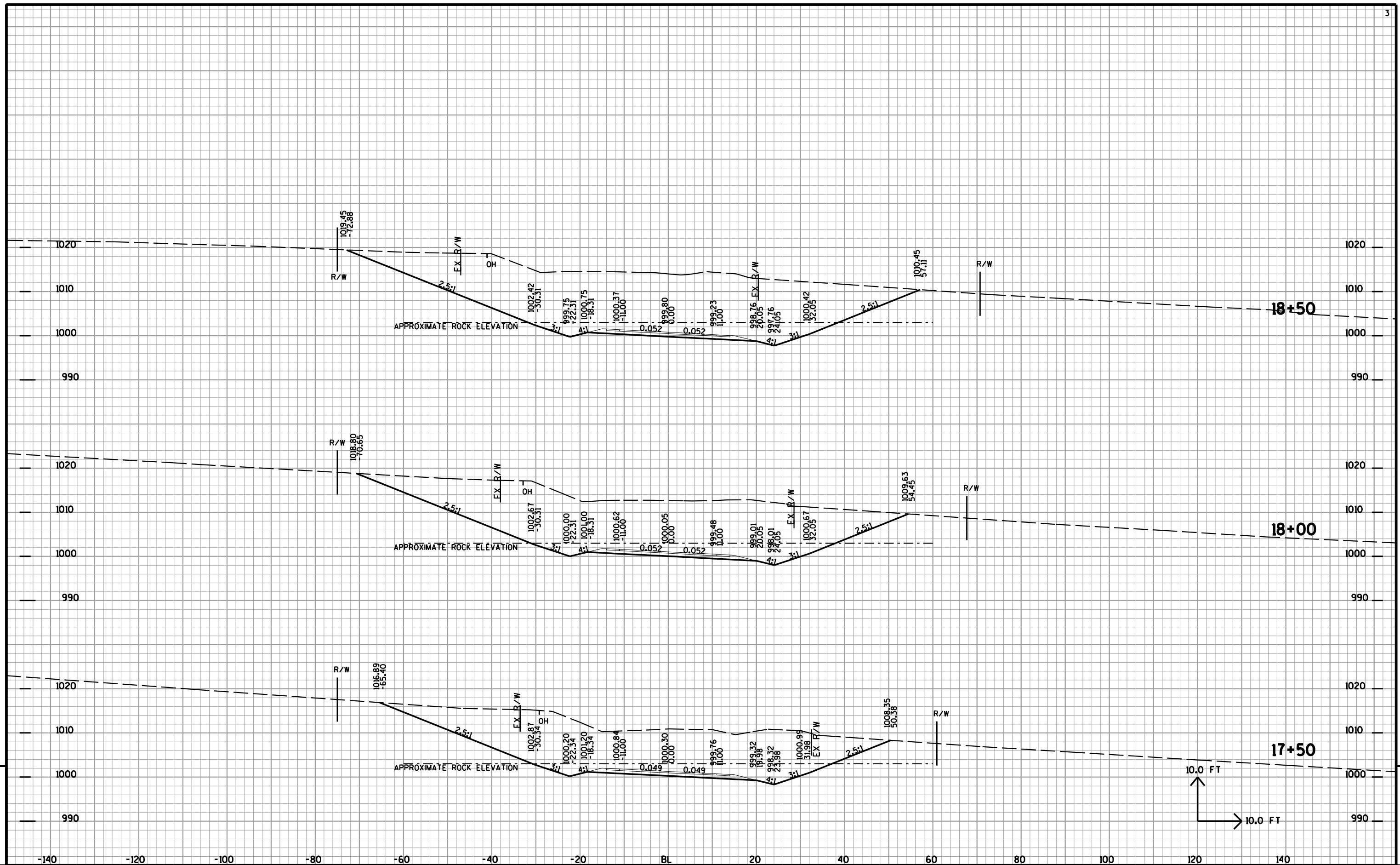
EARTHWORK DATA - PATTERSON ROAD							
STATION	END AREA			VOLUME			MASS HAUL
	COMMON SF	ROCK SF	FILL SF	COMMON CY	ROCK CY	FILL CY	
13+50.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
14+00.00	73.15	0.00	0.00	67.73	0.00	0.00	67.73
14+50.00	165.58	0.00	0.00	221.05	0.00	0.00	288.78
14+79.00	194.92	0.00	0.00	193.60	0.00	0.00	482.38
15+00.00	231.59	0.00	0.00	165.87	0.00	0.00	648.24
15+50.00	348.80	0.00	0.00	537.40	0.00	0.00	1185.64
16+00.00	446.52	29.55	0.00	736.41	30.10	0.00	1922.05
16+50.00	472.48	120.20	0.00	850.93	152.52	0.00	2772.98
17+00.00	599.28	154.20	0.00	992.37	279.48	0.00	3765.35
17+49.90	794.90	176.10	0.00	1288.33	335.74	0.00	5053.67
18+00.00	1009.40	193.40	0.00	1673.99	377.10	0.00	6727.66
18+49.90	1136.99	211.10	0.00	1983.42	411.17	0.00	8711.08
19+00.00	1190.78	229.10	0.00	2159.65	449.25	0.00	10870.74
19+50.00	1331.94	415.60	0.00	2335.85	656.64	0.00	13206.59
20+00.00	9.98	32.70	93.05	1242.52	456.60	107.70	14341.41
20+50.00	1028.98	283.70	0.00	962.00	322.26	107.70	15195.72
21+00.00	595.68	317.00	0.00	1504.31	611.82	0.00	16700.03
21+50.00	371.17	211.00	0.00	895.23	537.78	0.00	17595.26
22+00.00	342.83	54.60	0.00	661.11	270.52	0.00	18256.37
22+50.00	278.05	0.00	0.00	574.89	55.61	0.00	18831.26
23+00.00	180.72	0.00	0.00	424.79	0.00	0.00	19256.05
23+50.00	159.69	0.00	0.00	315.19	0.00	0.00	19571.24
23+62.00	172.49	0.00	0.00	73.82	0.00	0.00	19645.06
24+00.00	161.27	0.00	0.00	234.87	0.00	0.00	19879.93
24+50.00	0.00	0.00	0.00	149.32	0.00	0.00	20029.25

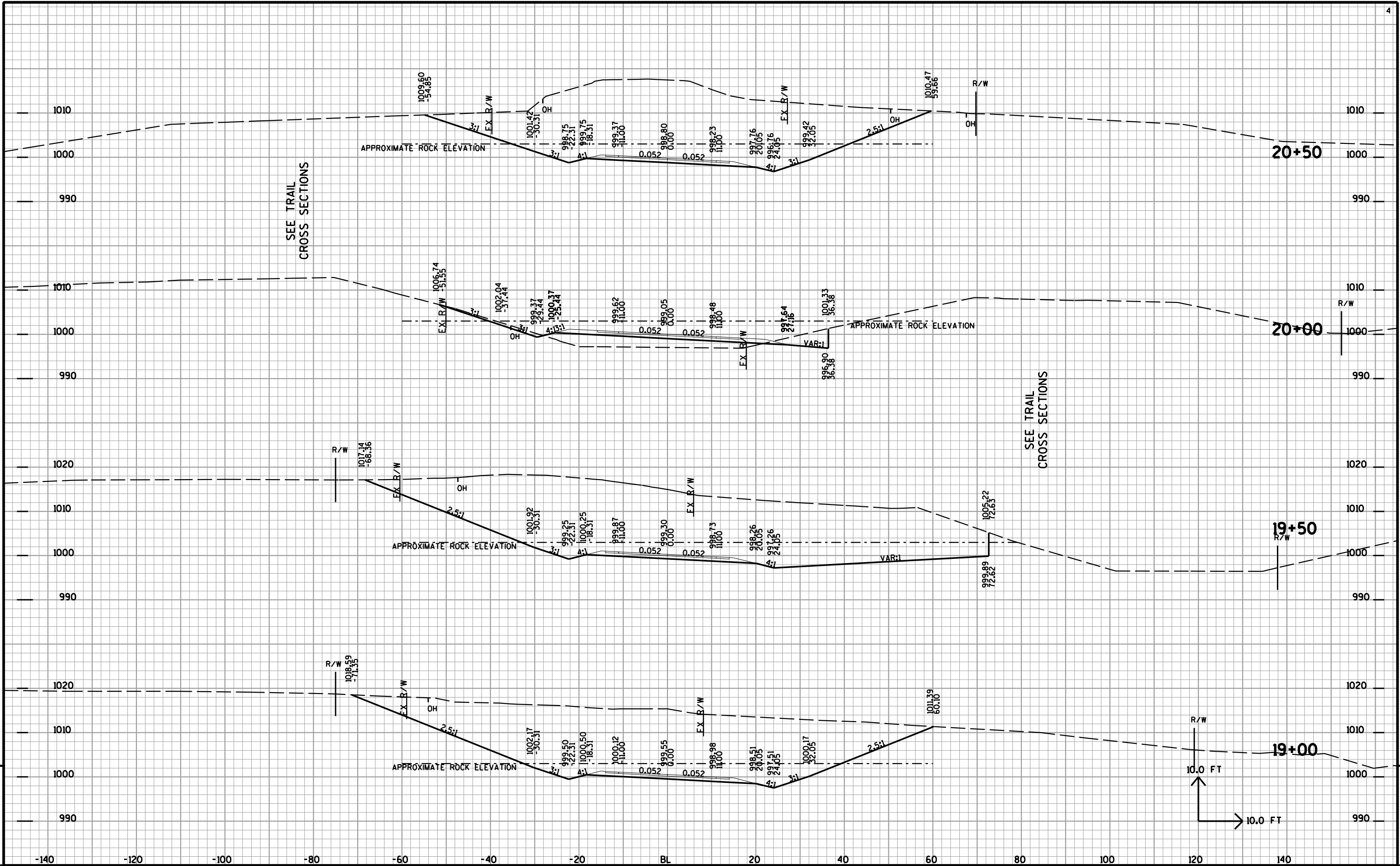
EARTHWORK DATA - TRAIL							
STATION	END AREA			VOLUME			MASS HAUL
	COMMON SF	ROCK SF	FILL SF	COMMON CY	ROCK CY	FILL CY	
101+50.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
102+00.00	88.62	98.45	1.27	82.06	100.27	1.47	80.59
102+50.00	483.40	255.89	16.03	529.65	360.90	20.02	590.21
102+84.51	0.00	0.00	0.00	308.93	179.89	12.81	886.33
103+00.00	0.00	0.00	47.11	0.00	0.00	16.89	869.44
103+50.00	141.21	39.76	38.84	130.75	40.50	99.48	900.71
104+00.00	94.55	77.50	9.99	218.30	119.43	56.52	1062.49
104+50.00	36.71	76.60	0.00	121.54	156.95	11.56	1172.47
105+00.00	0.00	0.00	0.00	33.99	78.02	0.00	1206.46

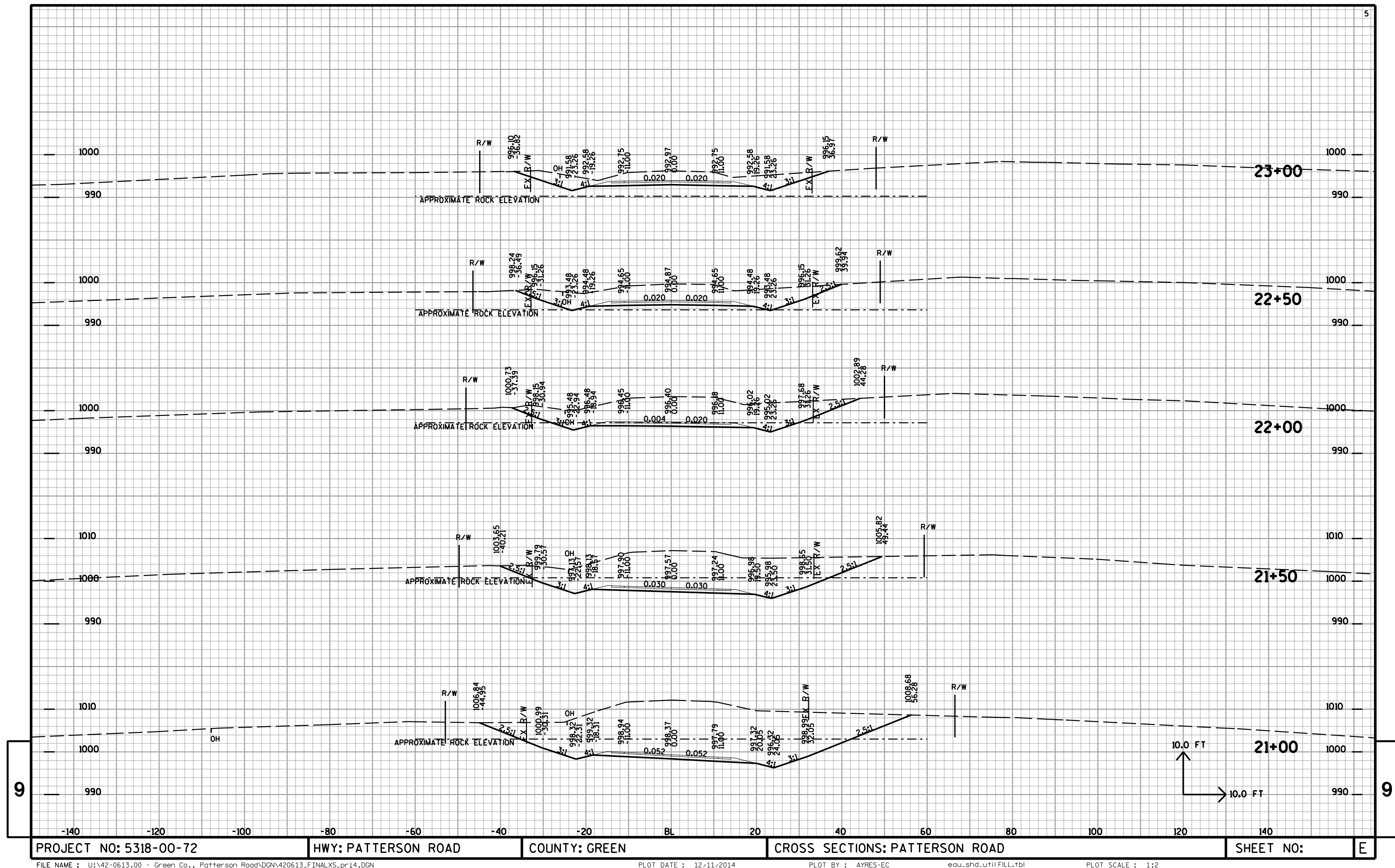


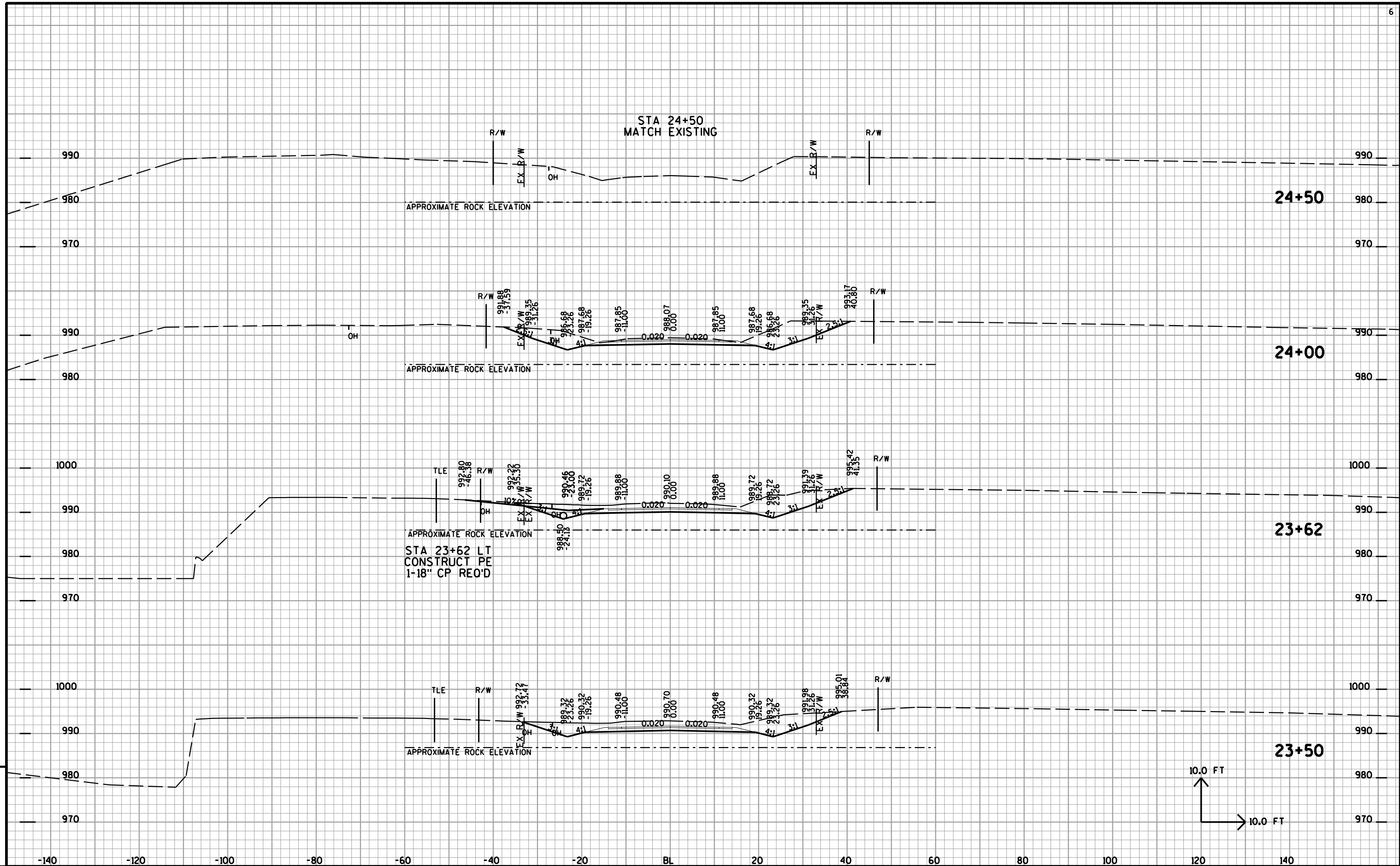


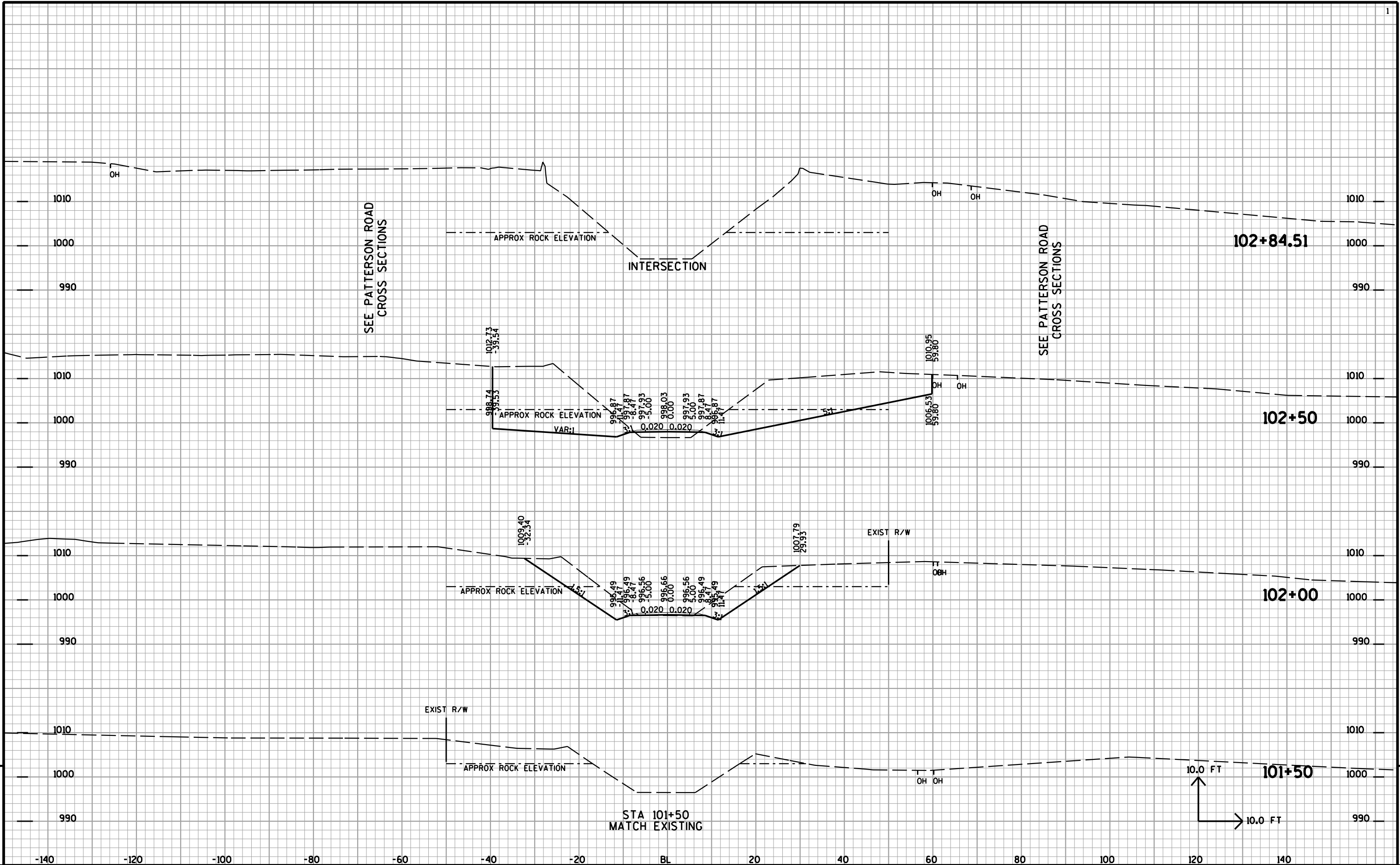


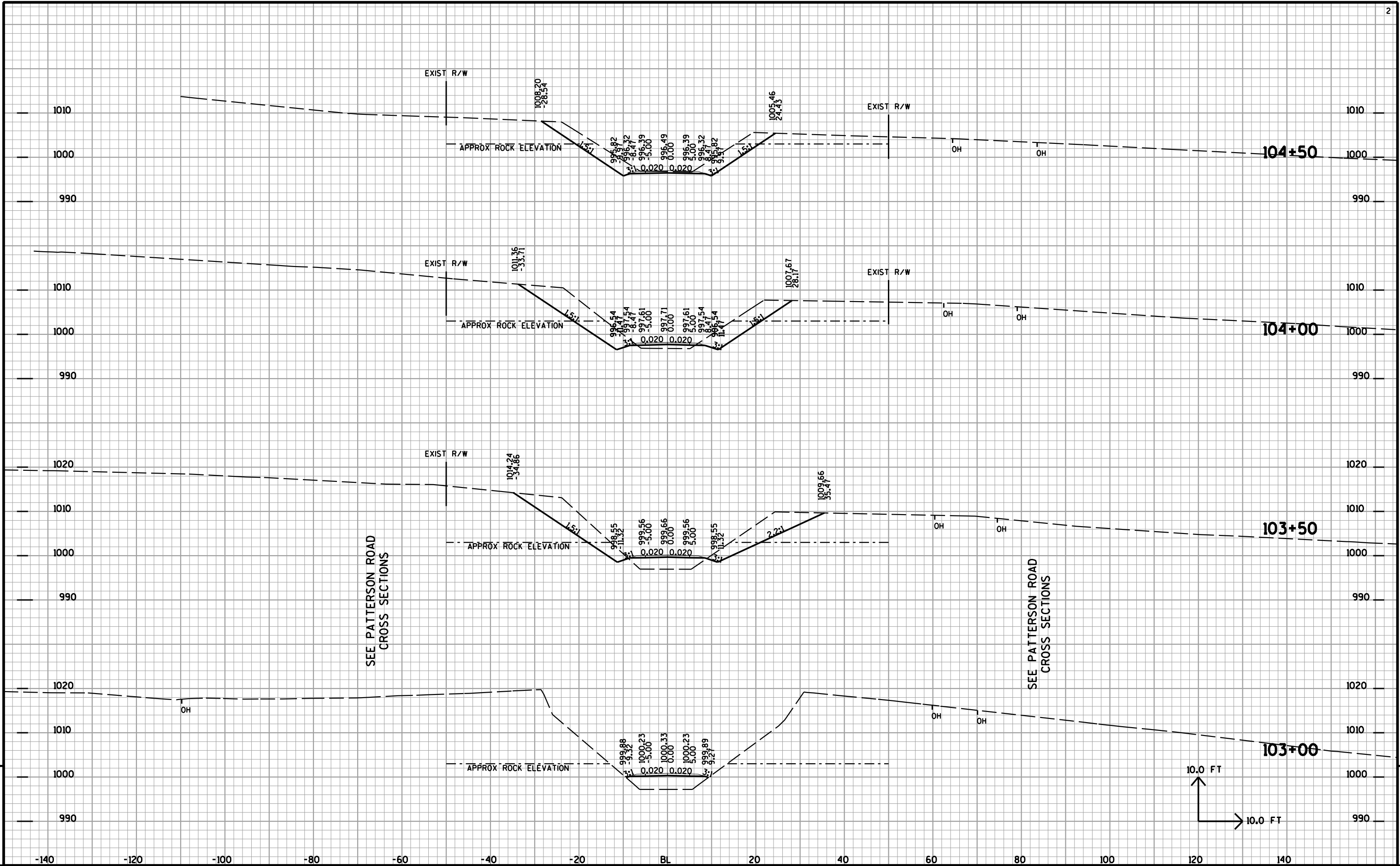




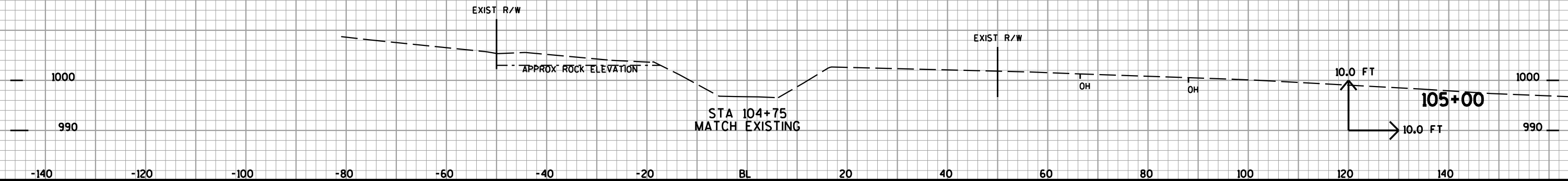








9



9



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