PROJECT WITH: S N 0 0-

JNTY:	
GR	
—	

WL	JUNE 2015	
	ORDER OF S	SHEETS

Typical Sections and Details (includes Erosion Control)

Estimate of Ouantities Section No. 3 Miscellaneous Ouantities Section No. 3 Right of Way Plat Section No. 4

Plan and Profile Section No. 5

Standard Detail Drawings Section No. 6 Section No. 7 Sign Plates

Section No. 8 Structure Plans Section No. 9 Computer Earthwork Data

Section No. 9 Cross Sections

TOTAL SHEETS = 78

DESIGN DESIGNATION

CONVENTIONAL SYMBOLS

LIMITED HIGHWAY EASEMENT

PROPOSED OR NEW R/W LINE

EXISTING RIGHT OF WAY

SLOPE INTERCEPT

REFERENCE LINE

EXISTING CULVERT

(Box or Pipe)

MARSH AREA

RAILROAD

FENCE

PROPOSED CULVERT

COMBUSTIBLE FLUIDS

WOODED OR SHRUB AREA

EDGE OF STREAM

CORPORATE LIMITS

PROPERTY LINE

LOT LINE

A.A.D.T. 2015

A.A.D.T. 2035

D.H.V. 2035

DESIGN SPEED

ESALS

PLAN

STATE OF WISCONSIN

DEPARTMENT OF TRANSPORTATION

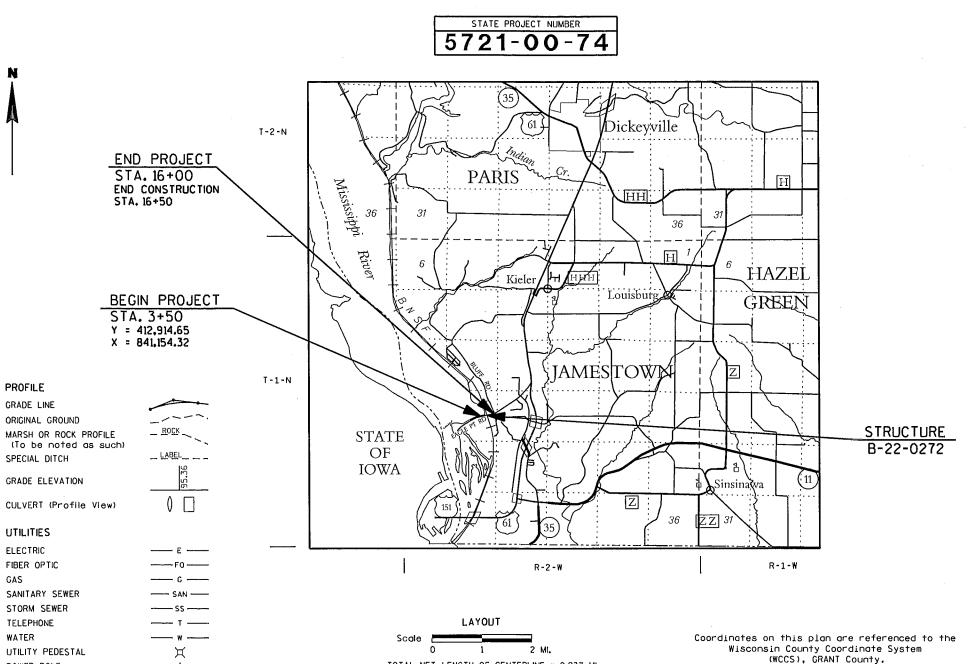
PLAN OF PROPOSED IMPROVEMENT

FEDERAL PROJECT STATE PROJECT PROJECT CONTRACT 5721-00-74 _

OF JAMESTOWN, EAGLE POINT ROAD TOWN

(BNSF RAILWAY COMPANY BRIDGE B-22-0272)

TOWN ROAD GRANT COUNTY



ACCEPTED FOR DATE ORIGINAL PLANS PREPARED BY: MSA DEVELOPMENT • ENVIRONMENTAL MINISCON ON WAGNER E-19220 BARABOO. STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION PREPARED BY MSA Professional Services, Inc. Surveyor MSA Professional Services, Inc. Designer Kjohnson Engineers, Inc.

GAS

WATER

POWER POLE

TELEPHONE POLE

240

250 N/A

50/50

30 mph

5%

4

TOTAL NET LENGTH OF CENTERLINE = 0.237 MI.

STANDARD ABBREVIATIONS

AC	•	ACRE	F/L	FLOW LINE	SALV	SALVAGED
AG		AGGREGATE	FT	FOOT	SAN	SANITARY SEWER
<		ANGLE	GN	GRID NORTH	SECT	SECTION
	SPH	ASPHALTIC	HR	HANDICAP RAMP	SHLDR	SHOULDER
AC		ASPHALT CEMENT	HT	HEIGHT	SW	SIDEWALK
AD		AVERAGE DAILY TRAFFIC	CWT	HUNDREDWEIGHT	S	SOUTH
	& B	BALLED AND BURLAPPED	HYD	HYDRANT	SB	SOUTHBOUND
Вм		BENCH MARK	IN DIA	INCH DIAMETER	SPECS	SPECIFICATIONS
CB		CATCH BASIN	INL	INLET	SQ	SQUARE
	OR C/L	CENTER LINE	ID	INSIDE DIAMETER	SF OR SO FT	SQUARE FEET
Č-		CENTER TO CENTER	I	INTERSECTION ANGLE	SY	SQUARE YARD
)NC	CONCRETE	ΙE	INVERT ELEVATION	SSPRC	STORM SEWER
CO		COUNTY	IP	IRON PIPE OR PIN		PIPE REINFORCED CONCRETE
CT		COUNTY TRUNK HIGHWAY	JCT	JUNCTION	STD	STANDARD
CY		CUBIC YARD	L	LENGTH OF CURVE	SDD	STANDARD DETAIL DRAWINGS
	JLV	CULVERT	LF	LINEAR FOOT	STH	STATE TRUNK HIGHWAYS
CP		CULVERT PIPE	LC	LONG CHORD OF CURVE	STA	STATION
	PRC	CULVERT PIPE	LCB	LONG CHORD BEARING	SS	STORM SEWER
		REINFORCED CONCRETE	LS	LUMP SUM	Ť	TANGENT
C	& G	CURB AND GUTTER	MH	MANHOLE	TEL	TELEPHONE
D		DEGREE OF CURVE	N	NORTH	TEMP	TEMPORARY
DH	ıv.	DESIGN HOUR VOLUME	Y	NORTH GRID COORDINATE	TLE	TEMPORARY LIMITED EASEMENT
_	A OR Ø	DIAMETER	0E	OUTLET ELEVATION	T	TON
	ST	DISTRICT	OL	OUT LOT	TC	TOP OF CURB
DW		DRIVEWAY	OD	OUTSIDE DIAMETER	TN	TOWN
E		EAST	ОН	OVERHEAD LINES	TRANS	TRANSITION
X		EAST GRID COORDINATE	PAVT	PAVEMENT	T	TRUCKS (percent of)
EB	3	EASTBOUND	PLE	PERMANENT LIMITED EASEMENT	TYP	TYPICAL
EL	.EC	ELECTRIC	PC	POINT OF CURVATURE	UNCL	UNCLASSIFIED
EL	OR ELEV	ELEVATION	PΙ	POINT OF INTERSECTION	USH	UNITED STATES HIGHWAY
ЕМ	(B	EMBANKMENT	PT	POINT OF TANGENCY	VAR	VARIABLE
EW	1	ENDWALL	PCC	PORTLAND CEMENT CONCRETE	VERT	VERTICAL
ES	SALS	EQUIVALENT SINGLE	LB	POUND	VC	VERTICAL CURVE
		AXLE LOADS	PE	PRIVATE ENTRANCE	VOL	VOLUME
ΕX	(C	EXCAVATION	R OR RAD	RADIUS	WM	WATER MAIN
EB	3S	EXCAVATION BELOW	RR	RAILROAD	wv	WATER VALVE
		SUBGRADE	R	RANGE	w	WEST
	CIST	EXISTING	₹ OR R/L	REFERENCE LINE	WB	WESTBOUND
EX		EXPANSION	REQD	REQUIRED	YD	YARD
F-	·F	FACE TO FACE	RT	RIGHT		
FE	RT	FERTILIZER	R / W	RIGHT-OF-WAY		
	-	ETEL D. ENTRANCE	DD	DOAD		

RUNOFF COEFFICIENT TABLE								
		HYDROLOGIC	SOIL TABLE					
	A	В	С	D				
	SLOPE RANGE %	SLOPE RANGE %	SLOPE RANGE %	SLOPE RANGE %				
LAND USE:	6 & OVER 6 & OVER 6 & OVER							
SIDE SLOPE- TURF	.25 .27 .28 .30 .32 .34 .36 .38							
PAVEMENT:		•	•					
ASPHALT		.70	95					
CONCRETE		.8095						
GRAVEL ROADS, SHOULDERS		.4060						

ROAD

THE RUNOFF COEFFICIENTS OF SURFACE DRAINAGE AT THE PROJECT SITES WILL NOT BE CHANGED FROM BEFORE TO AFTER CONSTRUCTION. THE TOTAL AREA OF THE PROJECT IS 5.37 ACRES. THE TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES IS 2.86 ACRES.

DESIGN CONTACT

MSA PROFESSIONAL SERVICES, INC. ATTN .: DANIEL H. WAGNER P.E. 1230 SOUTH BOULEVARD BARABOO, WI 53913 PHONE: 608-355-8952 dwagner@msa-ps.com

DNR LIAISON

DEPARTMENT OF NATURAL RESOURCES ANDY BARTA ENVIRONMENTAL IMPACT SPECIALIST 3911 FISH HATCHERY ROAD FITCHBURG, WI 53711 PHONE: 608-275-3308 andrew.barta@wisconsin.gov

RAILROAD CONTACT

BNSF RAILROAD COMPANY
ATTN: CALVIN NUTT, MANAGER OF PUBLIC PROJECTS 80 44th AVENUE NE MINNEAPOLIS, MN 55421 PHONE: 763-782-3495 Calvin.Nutt@BNSF.com

UTILITIES

OVERHEAD TELEPHONE: FRONTIER COMMUNICATIONS ATTN: DAVID DAY 124 EAST LINCOLN WAY MORRISON, IL 61270 PHONE (OFFICE): 815-772-2078 PHONE (MOBILE): 815-499-8715 david.day@ftr.com

OVERHEAD ELECTRIC: WISCONSIN POWER & LIGHT CO. ATTN: JASON HOGAN 4902 N. BILTMORE LANE SUITE 1000 MADISON, WI 53718 PHONE (OFFICE): 608-458-4871 PHONE (MOBILE): 608-395-7395 jasonhogan@alliantenergy.com

* - NOT A MEMBER OF DIGGERS HOTLINE.



GENERAL NOTES

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS SHALL BE FERTILIZED, SEEDED AND MULCHED AS DIRECTED BY THE ENGINEER. OVERSOW PERMANENT SEEDING AREAS WITH TEMPORARY SEED AT 1.5 LBS. PER 1000 SQUARE FEET.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE AREA THAT ARE

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO USGS NAVD 88 DATUM. A DISK AT THE SW CORNER OF EXISTING BRIDGE IS ELEVATION 636.95.

THE 3½" ASPHALTIC SURFACE SHALL CONSIST OF A 1½" UPPER LAYER AND A 1½" LOWER LAYER. USE $\frac{1}{2}$ " NOMINAL AGGREGATE.

SILT FENCE TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER AND IN PLACE PRIOR TO CONSTRUCTION OR BRIDGE REMOVAL.

THE WISCONSIN DEPARTMENT OF TRANSPORTATION WILL FURNISH THE CONTRACTOR A MONUMENT WHICH SHALL BE SET IN THE STRUCTURE AS DESIGNATED BY THE ENGINEER.

WHEN THE QUANTITY OF THE ITEM OF BASE LAYER IS MEASURED FOR PAYMENT BY THE TON. THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

PROJECT NO:5721-00-74

HWY: TOWN ROAD

COUNTY: GRANT

GENERAL NOTES, ABBREVIATIONS & UTILITIES

SHEET

FILE NAME: P:\90s\93\930816\caddWDOT\Planshts\930816GN.dgn

FE

FIELD ENTRANCE

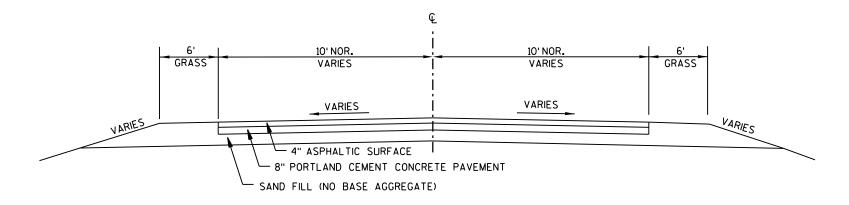
PLOT DATE: 3/24/2015

PLOT NAME :

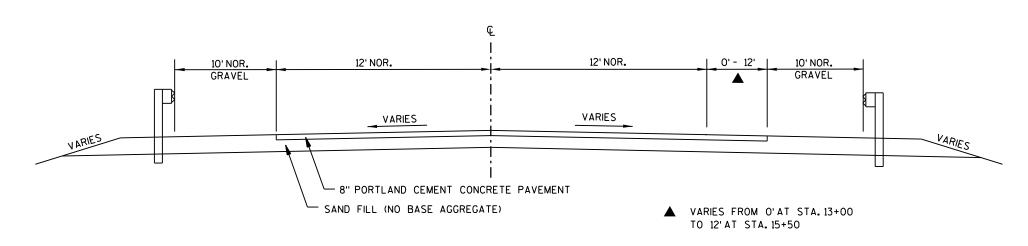
PLOT SCALE : 1:10

WISDOT/CADDS SHEET 42



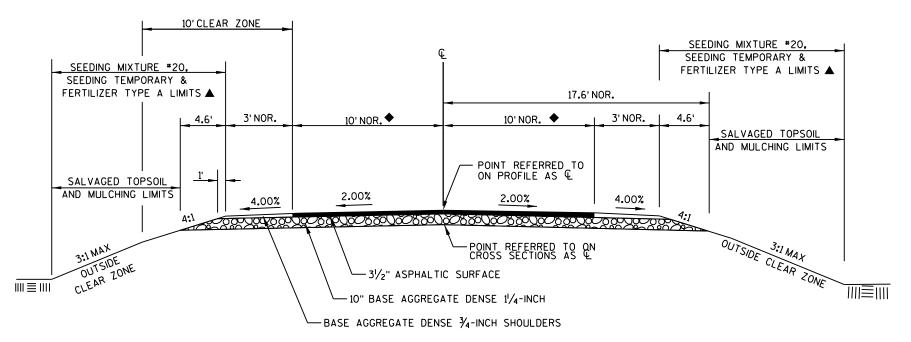


WEST APPROACH TYPICAL EXISTING SECTION



EAST APPROACH TYPICAL EXISTING SECTION

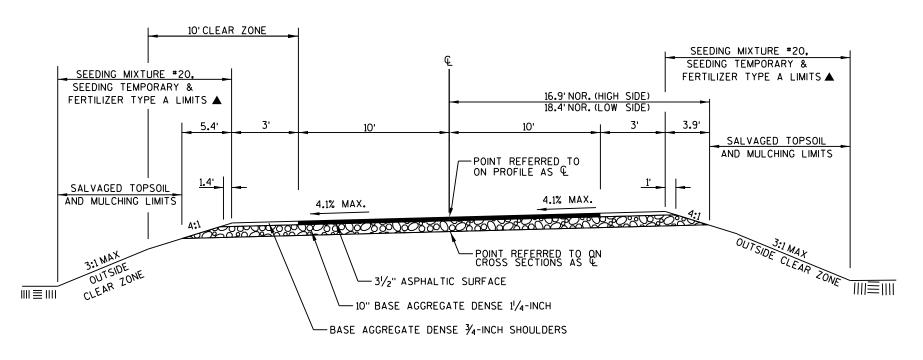
PROJECT NO:5721-00-74 HWY:TOWN ROAD COUNTY:GRANT TYPICAL EXISTING SECTIONS SHEET E



♦ WIDEN LEFT & RIGHT LANE PAVEMENT WIDTHS AT PROJECT END FROM 10'AT STATION 14+50 TO 12'ON LEFT AND 24'ON RIGHT AT STA. 16+00 TO MATCH EXISTING 12'LANE WIDTHS AND 12' RIGHT HAND CLIMBING LANE.

TYPICAL FINISHED SECTION

▲ DO NOT PLACE FERTILIZER WITHIN 20 FEET OF ANY WETLANDS.



TYPICAL FINISHED SUPERELEVATED SECTION

COUNTY: GRANT

SUPERELEVATION CAN BE IN EITHER DIRECTION

HWY: TOWN ROAD

TYPICAL FINISHED SECTIONS & DETAILS

PLOT BY : bhalley

PLOT SCALE : 1:8

WISDOT/CADDS SHEET 42

Ε

SHEET

FILE NAME: P:\90s\93\930816\caddWDOT\Planshts\930816FXC.dgn

PLOT DATE: 1/16/2015

PLOT NAME :

930816FXC.dgn 1/16/2015 3:57:01 PM bhalley

PROJECT NO:5721-00-74

CLASS I. TYPE B. NORMAL (SEE EROSION CONTROL PLAN) TYPICAL FINISHED SECTION WITH MGS BEAM GUARD DETAIL

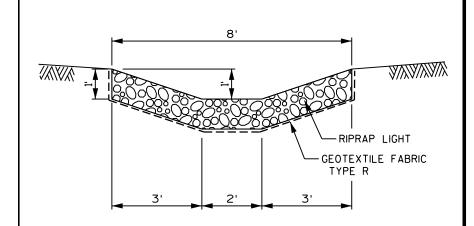
10:1 OR FLATTER-

EROSION MAT,

SEE MISCELLANEOUS QUANTITY SHEET FOR BEAM GUARD LOCATIONS. SEE STANDARD DETAIL DRAWINGS FOR ADDITIONAL INFORMATION. PAVED SHOULDER WIDTH VARIES FROM 2'AT STRUCTURE TO 5' AT END OF EAT BEAM GUARD SECTION.

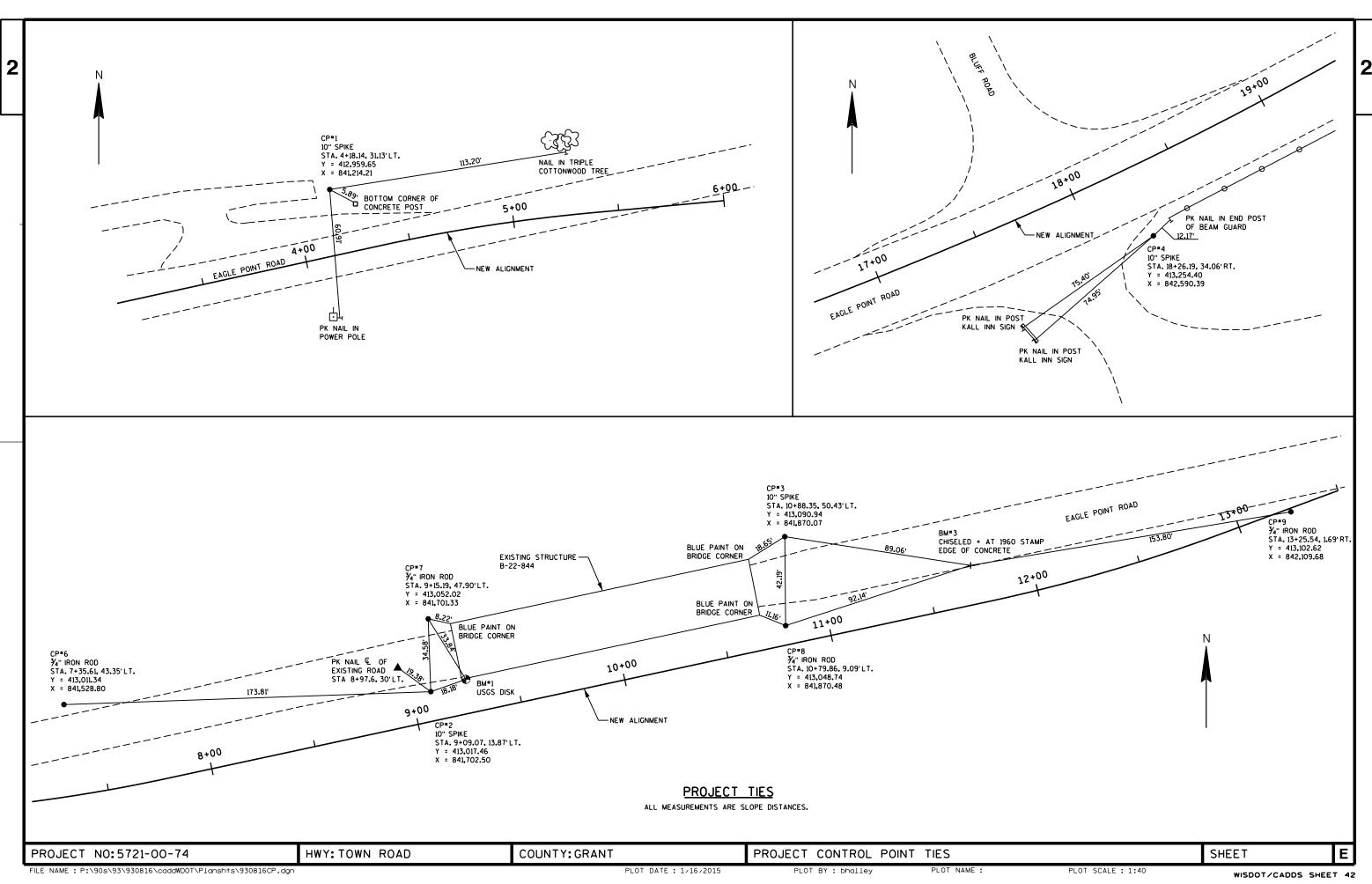
SHOULDER NORMAL

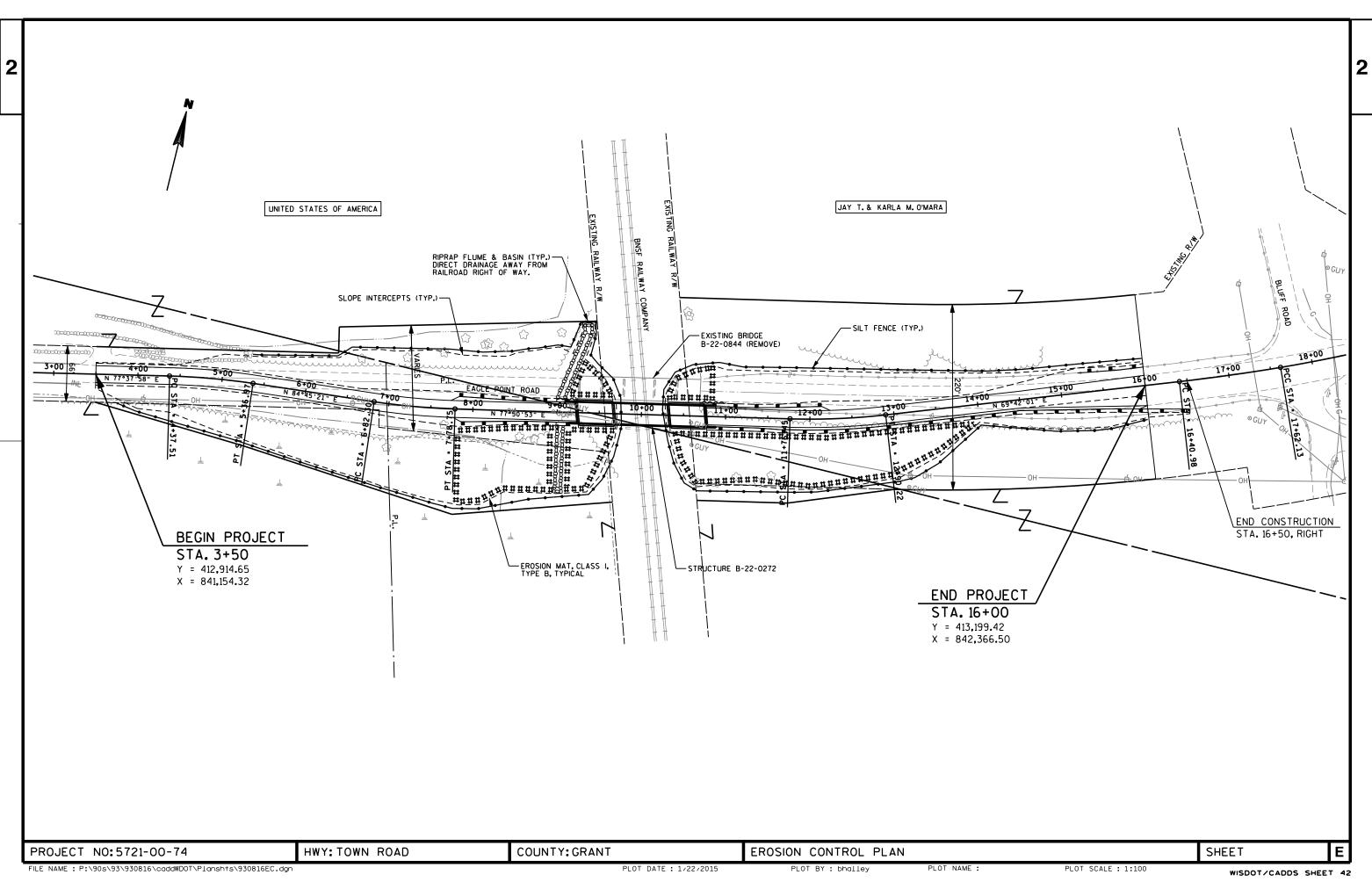
VARIES

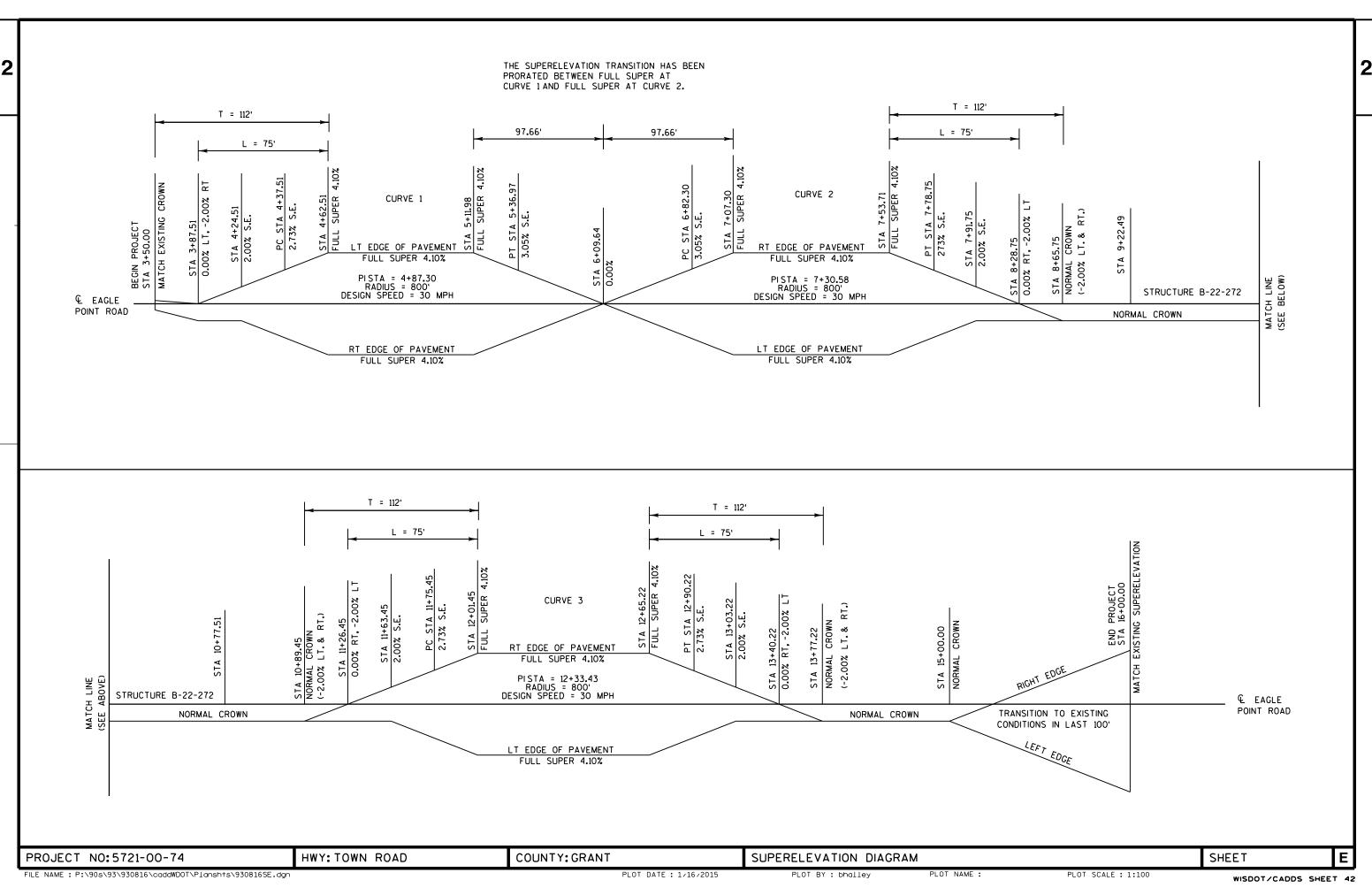


RIPRAP FLUME DETAIL CROSS SECTION

AT TOP OF RIPRAP FLUME PLACE 3.5" THICK ASPHALTIC PAD, 12'LONG BY 4'WIDE (MINIMUM). SHAPE TO DRAIN WATER FROM BRIDGE DECK INTO RIPRAP FLUME. PLACE BOTTOM OF FLUME OUTSIDE RAILROAD R/W.

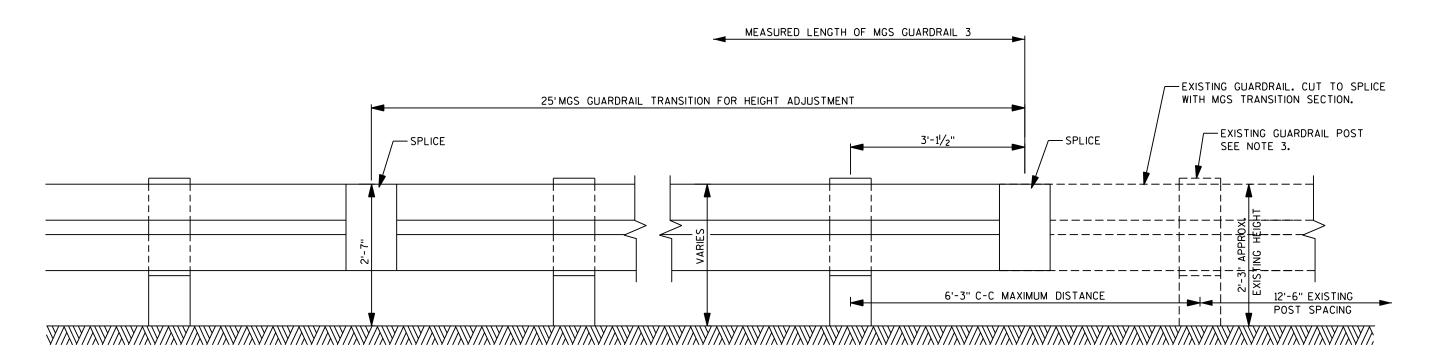






2

12



NOTES:

- 1. LOCATION OF SPLICE TO BE DETERMINED IN THE FIELD WITHIN 6'-3" OF PLAN LOCATION.
- 2. THE SPLICE SHALL BE LOCATED SO THAT THERE IS NO MORE THAN 6'-3" SPACE BETWEEN THE LAST EXISTING POST AND THE NEXT MGS POST.
- 3. INSTALL SUPPLEMENTAL POST ON EXISTING BEAM GUARD SIDE OF SPLICE IF NECESSARY TO MEET 6'-3" C-C MAXIMUM SPACING. PAYMENT INCLUDED WITH MEASURED LENGTH OF MGS GUARDRAIL 3.

MGS GUARDRAIL CONNECTION TO EXISTING GUARDRAIL DETAIL

NOT TO SCALE

PROJECT NO:5721-00-74 HWY:TOWN ROAD COUNTY:GRANT CONSTRUCTION DETAILS SHEET **E**

PLOT BY: bhalley

DATE 24 LINE	MAR15	E	ESTIMATE	OF QUAN	ITITIES 5721-00-74
NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	QUANTI TY
0010	201. 0105	Clearing	STA	12.000	12.000
0020 0030	201. 0205 203. 0200	Grubbing Removing Old Structure (station) 01.	STA LS	12. 000 1. 000	12. 000 1. 000
		STA. 9+97, 30' LT			
0040	203. 0225. 9	S Debris Containment (structure) 01.	LS	1. 000	1. 000
0050	204. 0100	B-22-0272 Removing Pavement	SY	2, 870. 000	2, 870. 000
			.		
0060	205. 0100	Excavation Common	CY	3, 021. 000	3, 021. 000
0070	206. 1000	Excavation for Structures Bridges (structure) 01. B-22-0272	LS	1. 000	1. 000
0800	208. 0100	Borrow	CY	28, 702. 000	28, 702. 000
0090	210. 0100	Backfill Structure	CY	170. 000	170.000
0100	213. 0100	Finishing Roadway (project) 01.	EACH	1. 000	1. 000
		5721-00-74			
0110	305. 0110	Base Aggregate Dense 3/4-Inch	TON	191. 000	191. 000
0120	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2, 570. 000	2, 570. 000
0130	455. 0605	Tack Coat	GAL	143.500	143. 500
0140	465. 0105	Asphal ti c Surface	TON	577. 000	577. 000
0150	502. 0100	Concrete Masonry Bridges	CY	370. 000	370. 000
0160	502. 3200	Protective Surface Treatment	SY	560. 000	560. 000
0170	503. 0136	Prestressed Girder Type I 36-Inch	LF	612.000	612.000
0180	505.0405	Bar Steel Reinforcement HS Bridges	LB	9, 950. 000	9, 950. 000
0190	505. 0605	Bar Steel Reinforcement HS Coated	LB	40, 710. 000	40, 710. 000
0200	506. 2605	Bridges Bearing Pads Elastomeric Non-Laminate	ed EACH	24. 000	24. 000
				24.000	24.000
0210	506. 4000	Steel Diaphragms (structure) 01.	EACH	9. 000	9. 000
0220	E11 1000	B-22-0272	C.E.	1 255 000	1 255 000
0220	511. 1200	Temporary Shoring (structure) 01. B-22-0272	SF	1, 255. 000	1, 255. 000
0230	516. 0500	Rubberized Membrane Waterproofing	SY	18. 000	18. 000
0240	550. 0500	Pile Points	EACH	22. 000	22. 000
0250	550. 1100	Piling Steel HP 10-Inch X 42 Lb	LF	1, 340. 000	1, 340. 000
0240	602 0000	Concrete Parrier Tomperery Presset	LF	350 000	250,000
0260	603. 8000	Concrete Barrier Temporary Precast Delivered	LF	350. 000	350. 000
0270	603. 8125	Concrete Barrier Temporary Precast	LF	350. 000	350. 000
		Installed			
0280	604.0500	Slope Paving Crushed Aggregate	SY	310.000	310.000
0290 0300	606. 0100 612. 0406	Riprap Light Pipe Underdrain Wrapped 6-Inch	CY LF	55. 000 180. 000	55. 000 180. 000
0300	012.0400	ripe underdrain wrapped o-inch	Lr'	180. 000	100.000
0310	614. 0150	Anchor Assemblies for Steel Plate Bea	am EACH	4. 000	4.000
		Guard			
0320	614. 0920	Sal vaged Rail	LF	1, 101. 000	1, 101. 000
0330 0340	614. 2300 614. 2500	MGS Guardrail 3 MGS Thrie Beam Transition	LF LF	662. 500 157. 600	662. 500 157. 600
0350	614. 2610	MGS Guardrail Terminal EAT	EACH	4. 000	4. 000
0360	619. 1000	Mobilization	EACH	1.000	1.000
0370	625. 0500	Sal vaged Topsoil	SY	11, 300. 000	11, 300, 000
0380 0390	627. 0200 628. 1504	Mulching Silt Fence	SY LF	16, 000. 000 2, 500. 000	16, 000. 000 2, 500. 000
0400	628. 1520	Silt Fence Maintenance	LF LF	2, 500. 000	2, 500. 000
0410	628. 1905	Mobilizations Erosion Control	EACH	2.000	2.000
0420	628. 1910	Mobilizations Emergency Erosion Contr		2.000	2.000
0430 0440	628. 2004 628. 6510	Erosion Mat Class I Type B Soil Stabilizer Type B	SY ACRE	5, 200. 000 1. 000	5, 200. 000 1. 000
			LF	20. 000	20. 000
0450	628. 7504	Temporary Ditch Checks	I F	Z(). U()()	

DATE 24	MAR15	EST	IMATE	0 F Q U A N		
LI NE NUMBER	ITEM	ITEM DESCRIPTION	UNI T	TOTAL	5721-00-74 QUANTI TY	
0460	629. 0205	Fertilizer Type A	CWT	13. 000	13. 000	
0400	630. 0120	Seeding Mixture No. 20	LB	340. 000	340. 000	
0480	630. 0200	Seeding Temporary	LB	600.000	600. 000	
0490	630. 0300	Seeding Borrow Pit	LB	140. 000	140. 000	
0500	633. 5100	Markers Row	EACH	15. 000	15. 000	
0300	000.0100	mar Rei 3 Row	LATON	13.000	13.000	
0510	634. 0612	Posts Wood 4x6-Inch X 12-FT	EACH	8. 000	8.000	
0520	637. 2230	Signs Type II Reflective F	SF	39. 500	39. 500	
0530	638. 2602	Removing Signs Type II	EACH	6.000	6.000	
0540	638. 3000	Removing Small Sign Supports	EACH	6.000	6.000	
0550	642. 5001	Field Office Type B	EACH	1.000	1.000	
0560	643. 0100	Traffic Control (project) 01. 5721-00-74	EACH	1. 000	1. 000	
0570	643. 0300	Traffic Control Drums	DAY	3, 600. 000	3, 600. 000	
0580	643. 0420	Traffic Control Barricades Type III	DAY	960. 000	960. 000	
0590	643. 0705	Traffic Control Warning Lights Type A	DAY	4, 800. 000	4, 800. 000	
0600	643. 0900	Traffic Control Signs	DAY	960. 000	960. 000	
0/10	6 4 F 0120	Contactile Febria Tune D	SY	225 000	225. 000	
0610	645. 0130 650. 4500	Geotextile Fabric Type R	LF	225.000		
0620 0630	650. 5000	Construction Staking Subgrade	LF LF	1, 095. 000 1, 095. 000	1, 095. 000 1, 095. 000	
0630	650. 6500	Construction Staking Base	LF LS	1, 095, 000	1, 095, 000	
0040	650. 6500	Construction Staking Structure Layout (structure) 01. B-22-0272	L3	1.000	1.000	
0650	650. 9910	Construction Staking Supplemental	LS	1. 000	1. 000	
0030	030. 9910		LJ	1.000	1.000	
		control (project) 01. 3721-00-74				
0660	650, 9920	Construction Staking Slope Stakes	LF	1, 095, 000	1, 095, 000	
0680	715. 0502		DOL			
0660 0670	650. 9920 690. 0250	Control (project) 01. 5721-00-74 Construction Staking Slope Stakes Sawing Concrete Incentive Strength Concrete Structures	LF LF	1, 095. 000 56. 000 2, 220. 000	1, 095. 000 56. 000 2, 220. 000	

455.0605 TACK COAT 201.0105 CLEARING 465.0105 ASPHALTIC SURFACE 201.0205 GRUBBING

CLEARING GRUBBING TACK COAT* LOCATION STATION STATION STA STA STATION STATION GAL 3+50 9+50 LT & RT 3+50.00 9+22.50 68.5 16+00 10+00 RT 10+77.50 16+00.00 74.5 TOTALS: ASPHALTIC PADS AT RIPRAP FLUMES TOTALS: 143.5

606.0100 RIPRAP LIGHT 645.0130 GEOTEXTILE FABRIC TYPE R

614.0920 SALVAGED RAIL

STATION

16+00

16+50

STATION

10+72.5

10+72.5 -

				RIPRAP	FABRIC
STATION	-	STATION	LOCATION	CY	SY
9+05	-	-	FLUME RIGHT	25	103
9+05	-	-	FLUME LEFT	30	122
			TOTALS:	55	225

LOCATION

TOTAL:

NOTES

NORTH OF EXISTING ROADWAY

SOUTH OF EXISTING ROADWAY

522

1101

NOTE: DO NOT GRUB WITHIN 20 FEET OF BURIED UTILITY LINES WITHOUT PERMISSION OF THE ENGINEER.

SY

1,270

1,600

2870

* TACK COAT APPLIED AT A RATE OF 0.05 GAL/SY

603.8000 CONCRETE BARRIER TEMPORARY PRECAST DELIVERED 603.8125 CONCRETE BARRIER TEMPORARY PRECAST INSTALLED 204.0100 REMOVING PAVEMENT

			TOTAL:	350	350
8+25, 23' LT	-	11+75, 23' LT	SOUTH EDGE OF EXISTING ROAD	350	350
STATION	-	STATION	LOCATION	LF	LF
				DELIVERED (1)	INSTALLED (1)
				BARRIER	BARRIER
				CONCRETE	CONCRETE

(1) - BARRIER TAPER SECTION REQUIRED ON EACH END OF CONCRETE BARRIER.

205.0100 EXCAVATION COMMON 208.0100 BORROW

STATION

3+50

10+69

STATION

9+25

16+00

			EXC. COMMON	FILL	EXPANDED FILL	BORROW	WASTE
STATION	-	STATION	CY	CY (1)	CY (2)	CY (3)	CY (3)
3+50	-	9+23 STAGE I	323	17022	22128	21806	0
3+50	-	9+23 STAGE II	2024	0	0	0	2024
10+78	-	16+00 STAGE I	426	5632	7322	6896	0
10+78	-	16+00 STAGE II	248	165	214	0	34
		TOTALS:	3021	22819	29664	28702	2058

(1) - NOT A BID ITEM - FOR INFORMATIONAL PURPOSES ONLY.

(2) - FILL EXPANSION 30%

(3) - STAGE II COMMON EXC. CANNOT BE USED FOR FILL ON STAGE I.

305.0110 BASE AGGREGATE DENSE 3/4-INCH 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH

			BASE	BASE
			AGGREGATE	AGGREGATE
			DENSE	DENSE
			3/4-INCH	1 1/4-INCH
STATION	-	STATION	TON	TON
3+50.00	-	9+22.50	118	1275
10+77.50	-	16+00.00	73	1295
		TOTALS:	191	2570

614.2300 MGS GUARDRAIL 3 614.2500 MGS THRIE BEAM TRANSITION 614.2610 MGS GUARDRAIL TERMINAL EAT

ASPHALTIC

SURFACE

TON

275

300

577

				MGS	THRIE	EAT	E	AT
				GUARDRAIL 3	BEAM	TERMINAL	POS1	NO. 1
STATION	-	STATION	LOCATION	LF	LF	EACH	STATION	OFFSET
7+83.6	-	8+36.7	LT	-	-	1	7+83.6	15.00' LT
7+86.3	-	8+39.5	RT	-	-	1	7+86.3	15.00' RT
8+36.7	-	8+74.2	LT	37.5	-	-		
8+39.5	-	8+77.0	RT	37.5	-	-		
8+74.2	-	9+13.6	LT	-	39.4	-		
8+77.0	-	9+16.4	RT	-	39.4	-		
10+83.6	-	11+23.0	LT	-	39.4	-		
10+86.4	-	11+25.8	RT	-	39.4	-		
11+23.0	-	11+60.5	LT	37.5	-	-		
11+25.8	-	16+50.0	RT	525	-	-		
11+60.5	-	12+14.3	LT	-	-	1	12+14.3	15.00' LT
15+21.9	-	15+75.0	LT	-	-	1	15+21.9	23.50' LT
15+75.0	-	16+00.0	LT	25	-	-		
			TOTALS:	662.5	157.6	4		

CONNECT MGS GUARDRAIL 3 TO EXISTING BEAM GUARD AT STA 16+00 LEFT AND 16+50 RIGHT. AT THESE CONNECTIONS TRANSITION FROM STANDARD HEIGHT OF GUARDRAIL IN A 25-FOOT LENGTH TO MATCH THE HEIGHT OF EXISTING BEAM GUARD. SEE "GUARDRAIL CONNECTION TO EXISTING GUARDRAIL DETAIL".

OFFSET GIVEN FOR EAT POST NO. 1 IS TO FACE OF RAIL. SEE SDD "MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)" AND CROSS SECTIONS.

PROJECT NO: 5721-00-74 **HWY: TOWN ROAD COUNTY: GRANT MISCELLANEOUS QUANTITIES SHEET NO:** Ε

PLOT NAME: FILE NAME : PLOT DATE: PLOT BY: ORIGINATOR : PLOT SCALE: 1:1 ORG DATE:

625.0500 SALVAGED TOPSOIL

627.0200 MULCHING

628.6510 SOIL STABILIZER TYPE B

629.0205 FERTILIZER TYPE A

630.0120 SEEDING MIXTURE NO. 20

630.0200 SEEDING TEMPORARY

630.0300 SEEDING BORROW PIT

STATION	-	STATION	LOCATION	SALVAGED TOPSOIL SY	MULCHING SY	SOIL STABILIZER ACRE	FERTILIZER CWT	SEEDING #20 LB	SEEDING TEMPORARY LB	SEEDING BORROW PIT LB
3+50	-	9+70	LT	2050	2050	0	1.45	63	63	-
3+50	-	9+70	RT	4250	2715	-	2.00	122	122	-
10+30	-	16+00	LT	1510	1510	0	1.10	48	48	-
10+30	-	16+00	RT	2470	270	-	1.70	74	74	-
BORROW PIT	-			-	9000	-	5.65	-	243	122
UNDIS	TRIBU	ITED		1020	455	0.3	1.10	33	50	18
1										
		TOTALS:		11300	16000	1	13.00	340	600	140

628.1504 SILT FENCE MAINTEN

628.1520 SILT FENCE MAINTENANCE

					FENCE	MAINT.
	STATION	-	STATION	LOCATION	LF	LF
_	13+50	-	9+70	LT & RT	1,225	1225
	10+25	-	16+00	LT & RT	1,275	1275
_				TOTALS:	2500	2500

628.2004 EROSION MAT CLASS I TYPE B

TOTALS:

628.7504 TEMPORARY DITCH CHECKS

TEMPORARY
DITCH
CHECKS
STATION LOCATION LF
UNDISTRIBUTED 20

TOTAL: 20

633.5100 MARKERS ROW

STATION	OFFSET	LOCATION	EACH	
3+50.00	32.69'	LT	1	
3+50.00	33.31'	RT	1	
5+36.97	75.00'	RT	1	
6+28.29	81.53'	LT	1	
6+32.11	50.75'	LT	1	
7+78.75	125.00'	RT	1	
9+44.26	109.07'	LT	1	
9+68.72	105.00'	RT	1	
10+41.38	140.00'	LT	1	
10+68.80	100.00'	RT	1	
11+76.45	100.00'	RT	1	
12+90.22	88.95'	RT	1	
13+11.21	130.26'	LT	1	
16+00.00	108.33'	LT	1	
16+00.00	111.68'	RT	1	
		TOTAL:	15	

634.0612 POSTS WOOD 4x6-INCH x 12-FT
637.2230 SIGNS TYPE II REFLECTIVE F
638.2602 REMOVING SIGNS TYPE II
638.3000 REMOVING SMALL SIGN SUPPORTS

5200

		SIZE	SIGN	WOOD	SIGNS REFECTIVE	REMOVING SIGNS TYPE II	REMOVING SMALL SIGN SUPPORTS	
OTATION	LOGATION	_						COMMENTO
STATION	LOCATION	WxH	CODE	EACH	SF	EACH	EACH	COMMENTS
2+00	RT	36" x 36"	W1-5	1	9.00	-	-	WINDING ROAD
2+00	RT	18" x 18"	W13-1P	-	2.25	-	-	ADVISORY SPEED PLAQUE - 30 MPH
9+20	LT	12" x 36"	W5-52L	1	3.00	-	-	OBJECT MARKER
9+23	RT	12" x 36"	W5-52R	1	3.00	-	-	OBJECT MARKER
9+23	LT	-	-	-	-	2	2	OBJECT MARKERS AT EXISTING BRIDGE
10+80	RT	12" x 36"	W5-52L	1	3.00	-	-	OBJECT MARKER
10+71	LT	-	-	-	-	2	2	OBJECT MARKERS AT EXISTING BRIDGE
10+77	LT	12" x 36"	W5-52R	1	3.00	-	-	OBJECT MARKER
11+76	LT	-	-	-	-	1	1	ADVISORY CURVE
13+45	RT	24" x 30"	R2-1	1	5.00	1	1	55 MPH REGULATORY
17+00	LT	36" x 36"	W1-5	1	9.00	-	-	WINDING ROAD
17+00	LT	18" x 18"	W13-1P	1	2.25	-	-	ADVISORY SPEED PLAQUE - 30 MPH
			TOTALS:	8	39.50	6	6	

PROJECT NO: 5721-00-74 HWY: TOWN ROAD COUNTY: GRANT MISCELLANEOUS QUANTITIES SHEET NO: E

FILE NAME : PLOT DATE : PLOT BY : PLOT NAME : ORG DATE : ORIGINATOR : PLOT SCALE : 1:1

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643.0100 TRAFFIC CONTROL 5721-00-74 643.0300 TRAFFIC CONTROL DRUMS

643.0420 TRAFFIC CONTROL BARRICADES TYPE III
643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A

643.0900 TRAFFIC CONTROL SIGNS

WARNING LIGHTS DRUMS BARRICADES SIGNS TYPE A LOCATION DAYS DAYS DAYS DAYS PROJECT 3600 4800 3600 960 4800 960

650.4500 CONSTRUCTION STAKING SUBGRADE

650.5000 CONSTRUCTION STAKING BASE

650.9910 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 5721-00-74

650.9920 CONSTRUCTION STAKING SLOPE STAKES

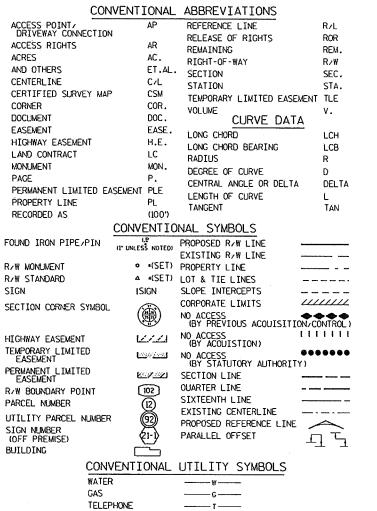
					SUPPLEMENTAL	SLOPE
			SUBGRADE	BASE	CONTROL	STAKES
STATION	-	STATION	LF	LF	LS	LF
3+50	-	9+22	572	572	-	572
10+77	-	16+00	523	523	-	523
P	ROJE	CT			1	
		TOTALS:	1095	1095	1	1095

690.0250 SAWING CONCRETE

STATION	LOCATION	LF
3+50	ROADWAY	20
16+00	ROADWAY	36
TOTAL:		56
		•

PROJECT NO: 5721-00-74 HWY: TOWN ROAD COUNTY: GRANT MISCELLANEOUS QUANTITIES SHEET NO:

FILE NAME : PLOT DATE : PLOT BY : PLOT NAME : ORG DATE : ORIGINATOR : PLOT SCALE : 1:1



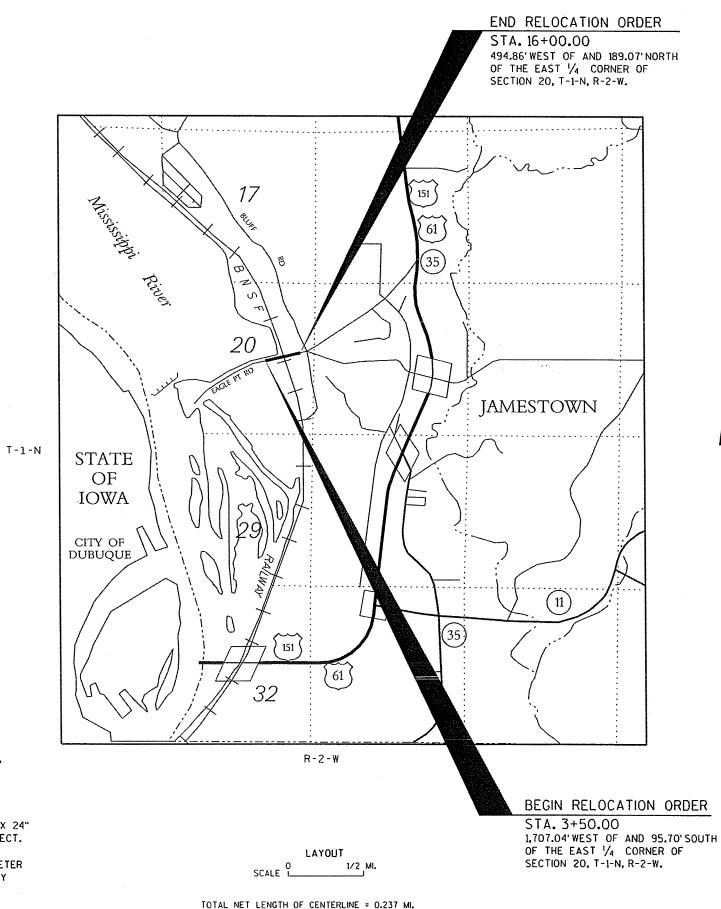
NOTES

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, GRANT COUNTY NAD 83 (2007) IN US SURVEY FEET, VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES, GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY 3/4" X 24" REBAR) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

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RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD".



R/W PROJECT NUMBER
5721-00-04

FEDERAL PROJECT NUMBER
PLAT OF RIGHT-OF-WAY REQUIRED FOR

TOWN OF JAMESTOWN, EAGLE POINT ROAD
(BNSF RAILWAY COMPANY BRIDGE B-22-0272)

TOWN ROAD GRANT COUNTY

CONSTRUCTION PROJECT NUMBER 5721-00-74

FRIENDSHIP, WI SURVE O1/15/15 Date Signature

ORIGINAL PLAT PREPARED BY:

MISCONSIN

GREGORY P.

RHINEHART

S-1478

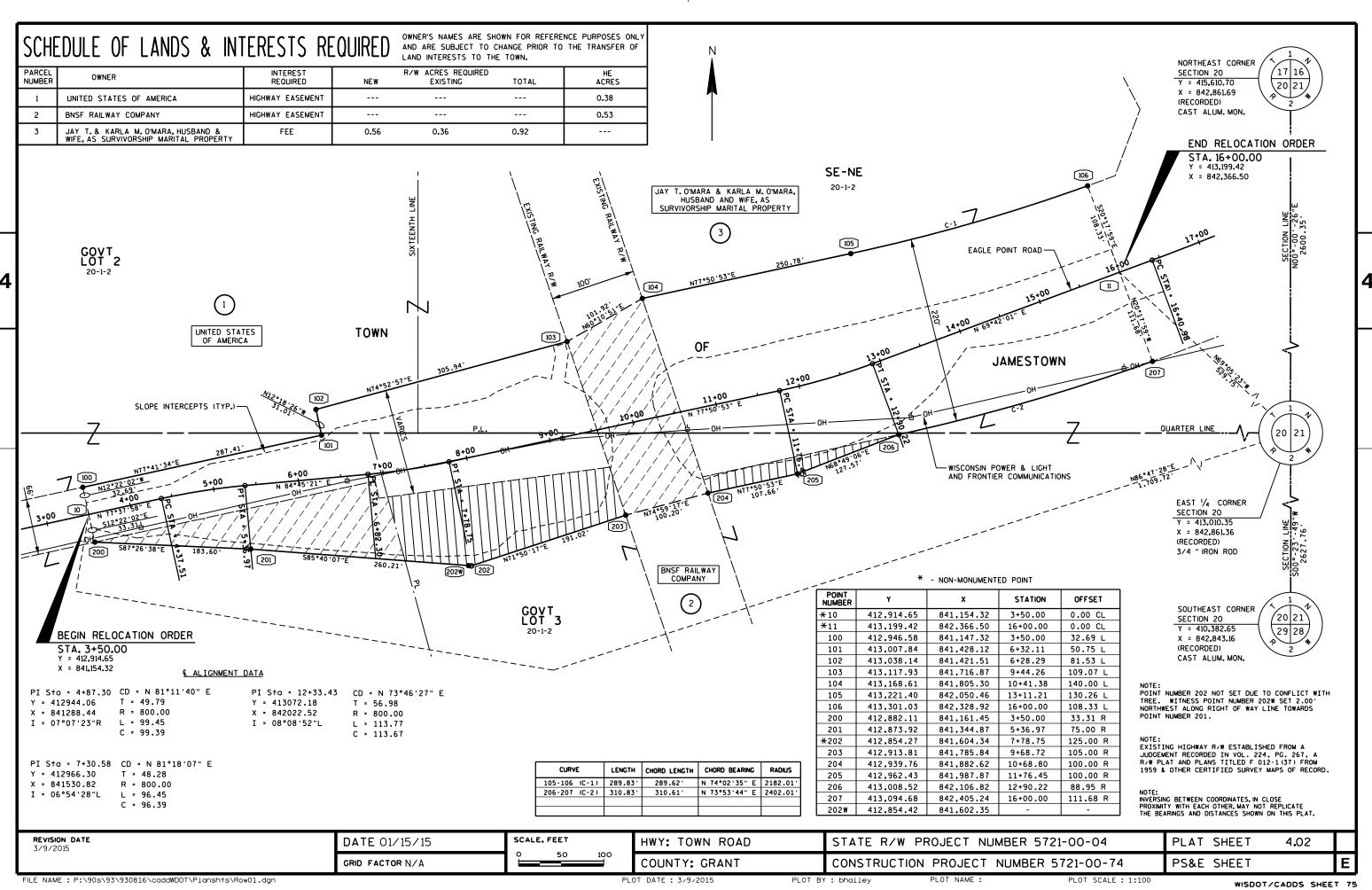
REVISION DATE 3/9/2015 N.C.

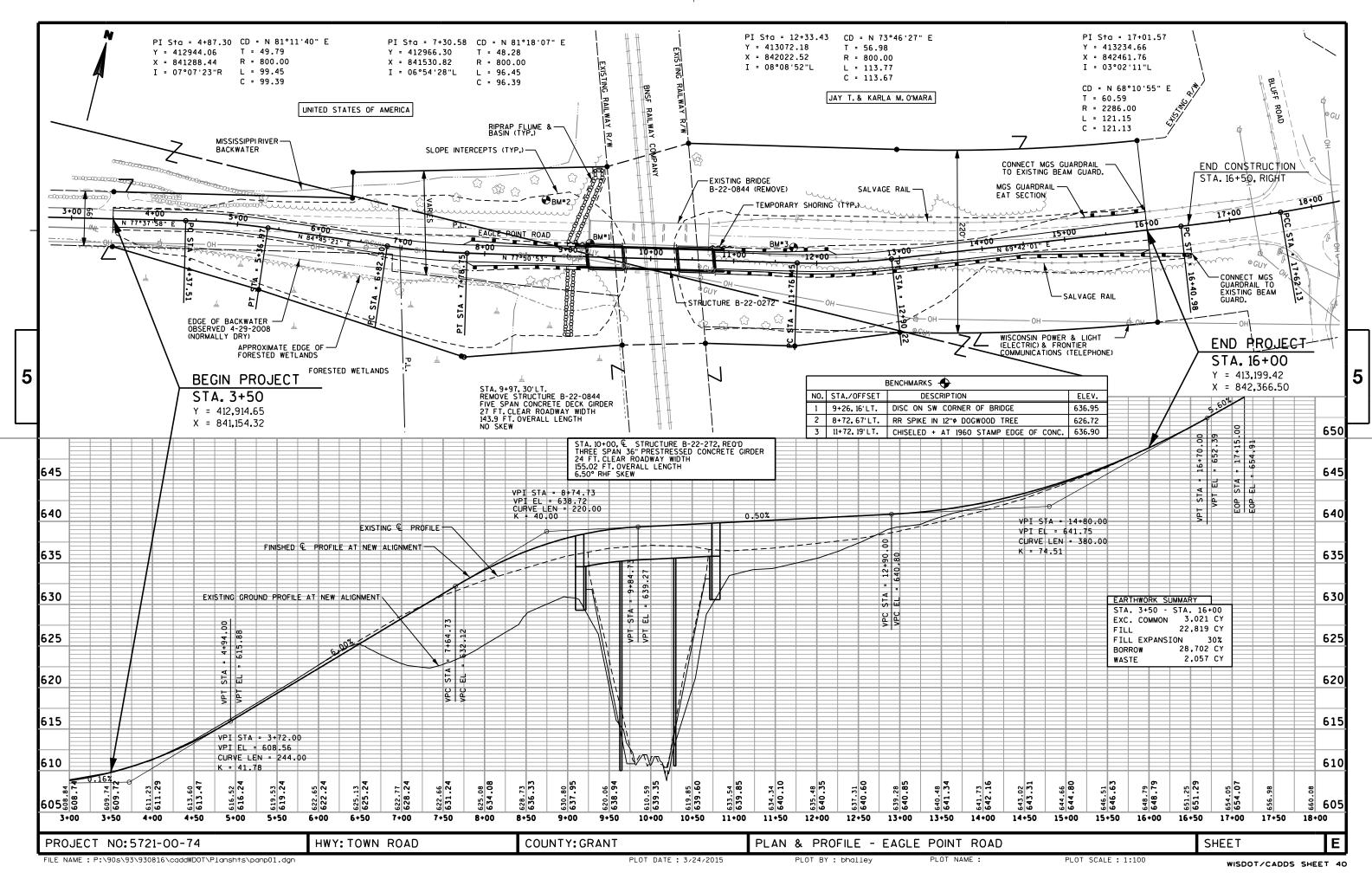
APPROVED FOR TOWN OF JAMESTOWN

DASEZ-10-15 Falls Secret 1
TOWN CHAIRPERSON

ELECTRIC TOWER

PLOT NAME :





Standard Detail Drawing List

	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
12403-10	NAME PLATE (STRUCTURES)
14B07-14A	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-14B	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6" CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-14C	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-14D	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-14E	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-14F	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-14G	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B42-03A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-03B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-03C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-02A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-02C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B45-03A	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-03B	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-03C	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-03D	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-03I	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
14B45-03J	MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)
15A01-11	MARKER POST FOR RIGHT-OF-WAY
15C06-07	SIGNING & MARKING FOR TWO LANE BRIDGES
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15D28-02	SIGNING & MARKING FOR TWO LANE BRIDGES TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS) TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D31-02	TRAFFIC CONTROL, TEMPORARY BYPASS ROADWAY

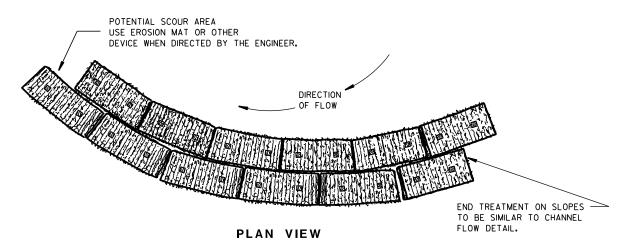
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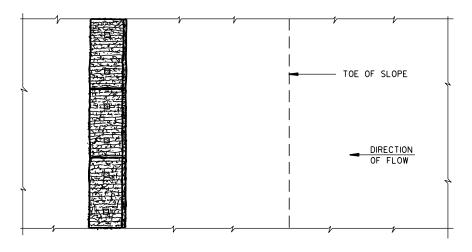
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

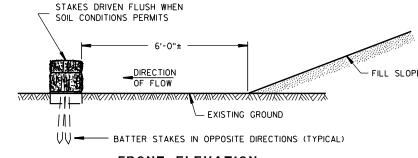
TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF **EROSION BALES / TEMPORARY** DITCH CHECKS

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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

6/04/02 /S/ Beth Connestro
CHIEF ROADWAY DEVELOPMENT ENGINEER

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TYPICAL APPLICATION OF SILT FENCE

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PLAN VIEW SILT FENCE AT MEDIAN SURFACE DRAINS



GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- 2 FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- 3 WOOD POSTS SHALL BE A MINIMUM SIZE OF 11/8" X 11/8" OF OAK OR HICKORY.
- 4) SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- (5) CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



TRENCH DETAIL



SILT FENCE TIE BACK

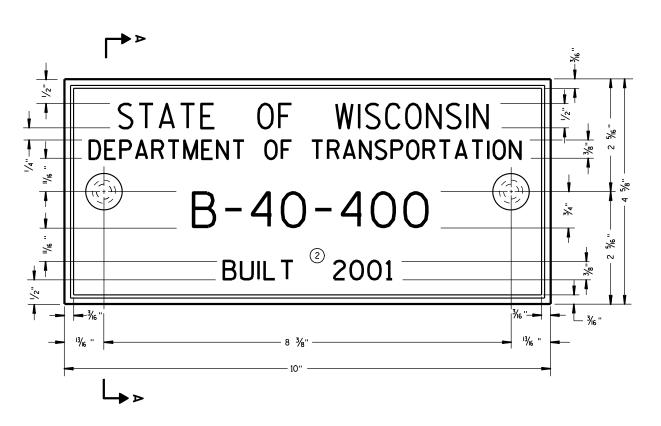
(WHEN REQUIRED BY THE ENGINEER)



SILT FENCE

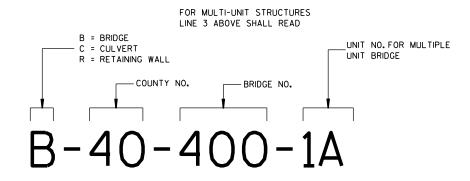
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TYPICAL NAME PLATE

(BRIDGES, CULVERTS, AND RETAINING WALLS)



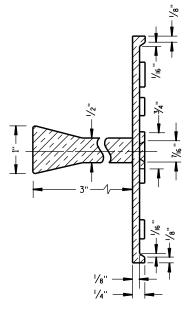
NUMBERING DESIGNATION MULTI-UNIT STRUCTURES

GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

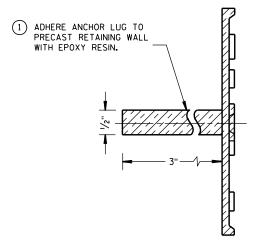
- 1 EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- (2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SPREAD OPEN SO THE TOP OF LUG IS 11/4" WIDE

SECTION A-A

ALTERNATE LUG



ALTERNATE LUG

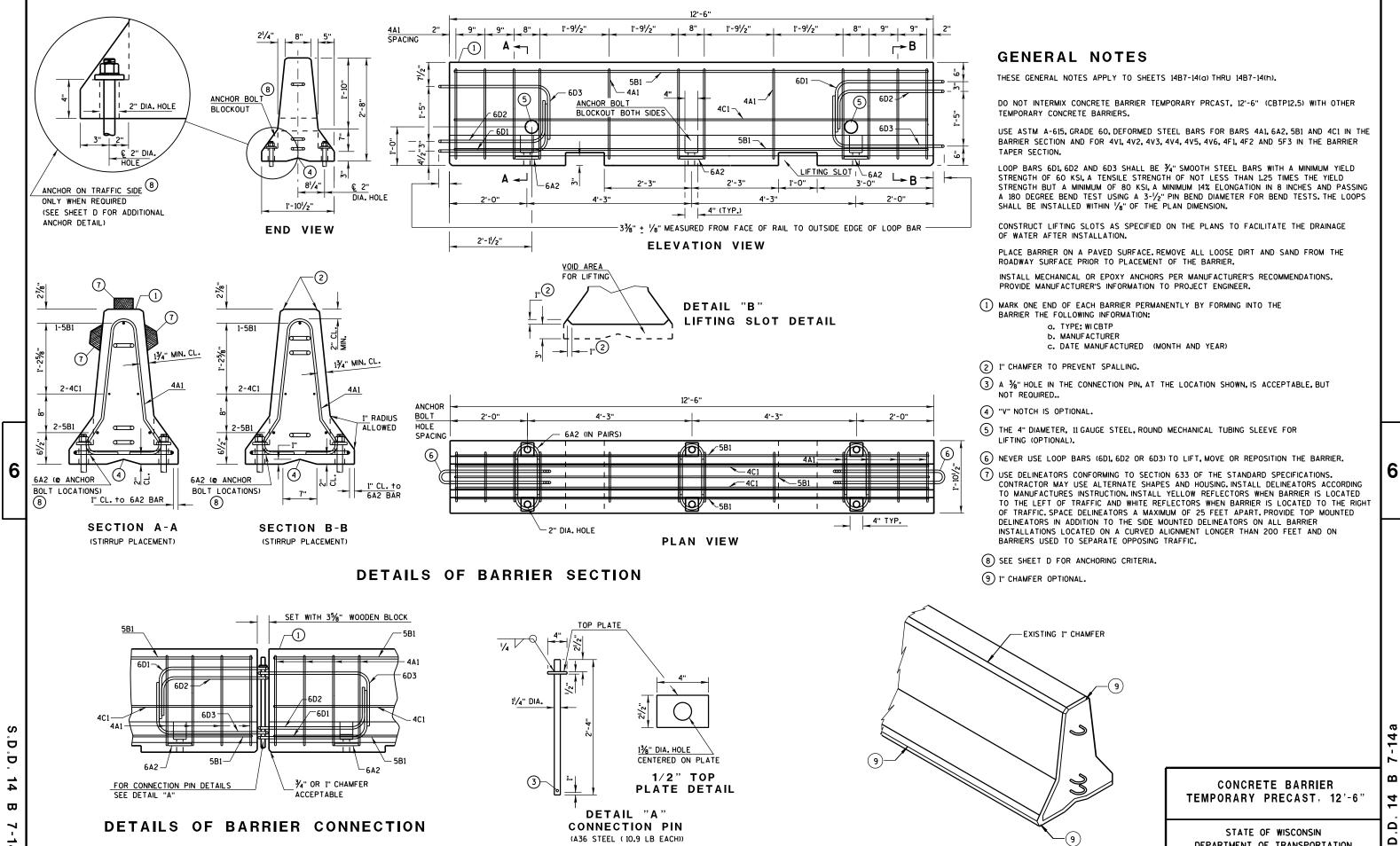
(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

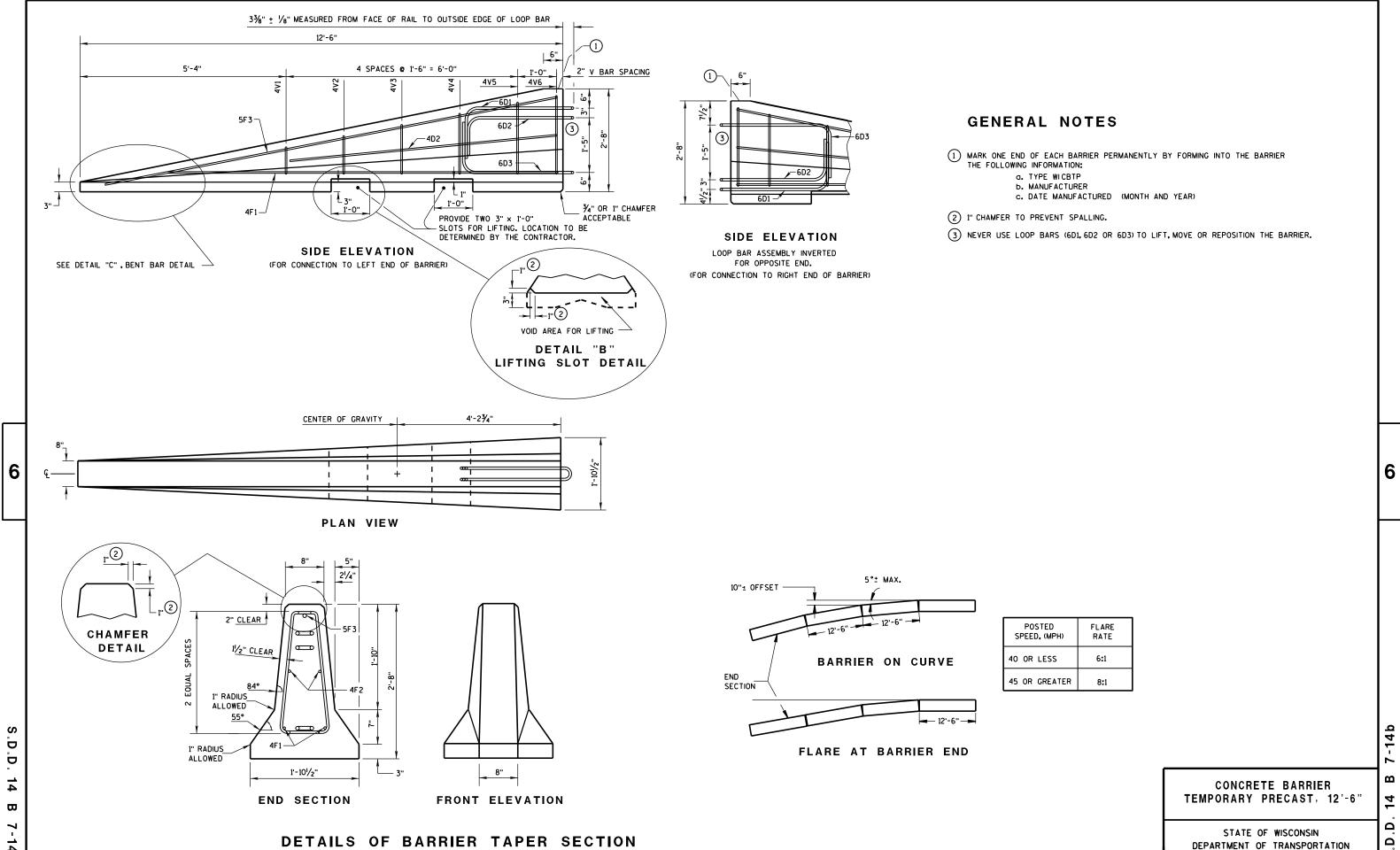
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

 D. 12 A 3-10



DEPARTMENT OF TRANSPORTATION



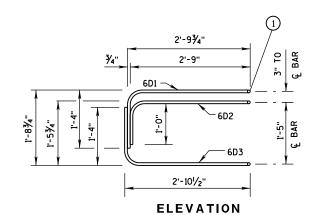
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1) NEVER USE LOOP BARS (6D1, 6D2 OR 6D3) TO LIFT, MOVE OR REPOSITION THE BARRIER.

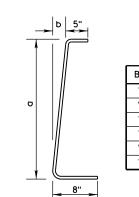
BARRIER TAPER SECTION BILL OF MATERIALS

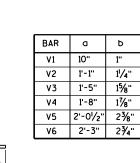
(PER 12'-6" BARRIER TAPER SECTION)

WENTE O BANNEN TALEN SECTION						
BAR	BAR SIZE	NO. OF BARS	LENGTH FT.			
4V1	4	2	1'-11"			
4V2	4	2	2'-2"			
4٧3	4	2	2'-6"			
4V4	4	2	2'-9"			
4V5	4	2	3'-2"			
4V6	4	2	3'-4"			
4F1	4	2	12'-0"			
4F2	4	2	7'-6"			
5F3	5	1	11'-9"			
L	LOOP ASSEMBLY					
6D1	6	1	8'-5"			
6D2	6	1	7'-7"			
6D3	6	1	8'-6"			
		•	•			



LOOP BAR ASSEMBLY





DETAIL "C" BENT BAR DETAIL

2" MIN. CLEAR

2" MIN. CLEAR

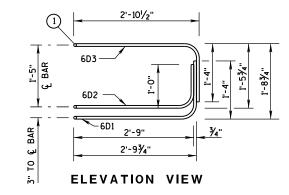
4V BARS
2 AT EACH SIZE REQUIRED
FOR STIRRUP ASSEMBLY

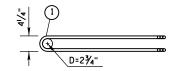
TAPER BARRIER SECTION

BARRIER SECTION BILL OF MATERIALS

(PER 12'-6" BARRIER SECTION)

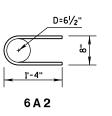
BAR	BAR SIZE	NO. OF BARS	LENGTH FT.
4A1	4	12	6'-0"
6A2	6	6	2'-11"
5B1	5	3	12'-2"
4C1	4	2	12'-2"
L	OOP AS	SSEMBL	Υ
6D1	6	2	8'-5"
6D2	6	2	7'-7"
6D3	6	2	8'-6"

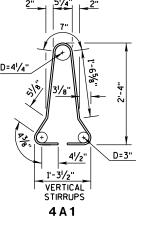




PLAN VIEW Loop bar assembly

(MARKED END SHOWN, INVERT FOR OTHER END)



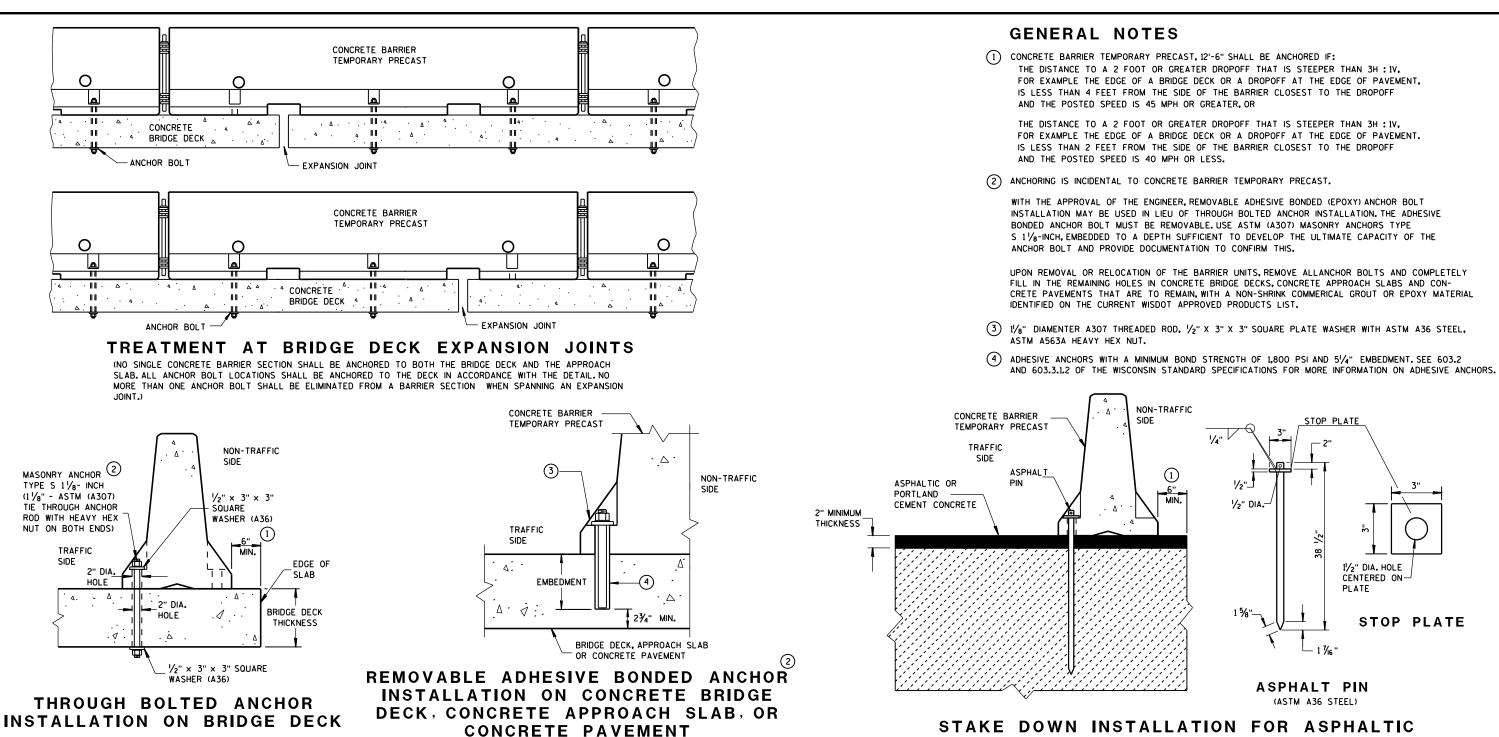


BARRIER SECTION

CONCRETE BARRIER
TEMPORARY PRECAST, 12'-6"

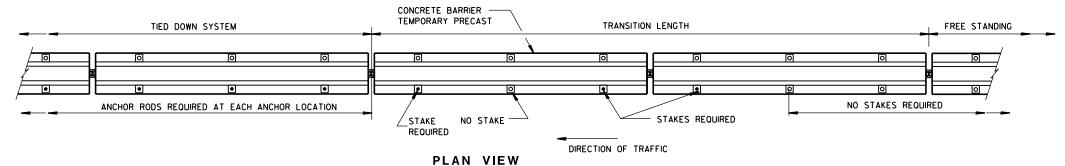
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

.D.D. 14 B 7-14c



STAKE DOWN INSTALLATION FOR ASPHALTIC OR PORTLAND CEMENT CONCRETE SURFACE

(STAKING IS INCIDENTAL TO CONCRETE BARRIER TEMPORARY PRECAST)



(DO NOT USE ON CONCRETE WITH AN ASPHALTIC OVERLAY)

FREE STANDING TRANSITION TO TIED-DOWN SYSTEM (PLACE TRANSITION IN A TANGENT SECTION OF BARRIER PARALLEL TO THE ROADWAY, IF TRANSITION OCCURS ON STRUCTURAL SLAB, ANCHOR AS SHOWN,)

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(DO NOTUSE ON CONCRETE BRIDGE DECK WITH ASPHALT OVERLAY)

STATE OF WISCONSIN

CONCRETE BARRIER

TEMPORARY PRECAST, 12'-6"

11/2" DIA. HOLE

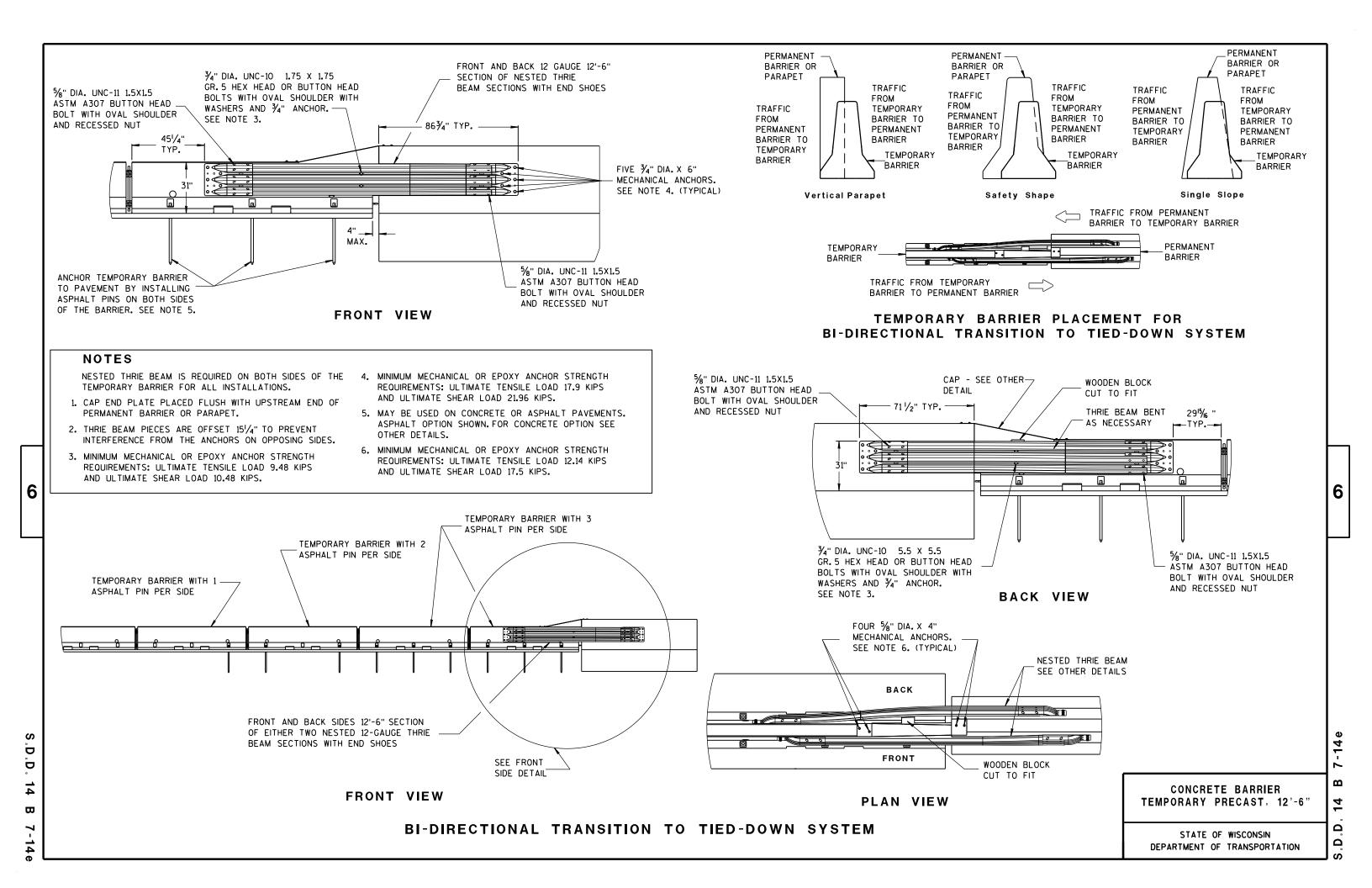
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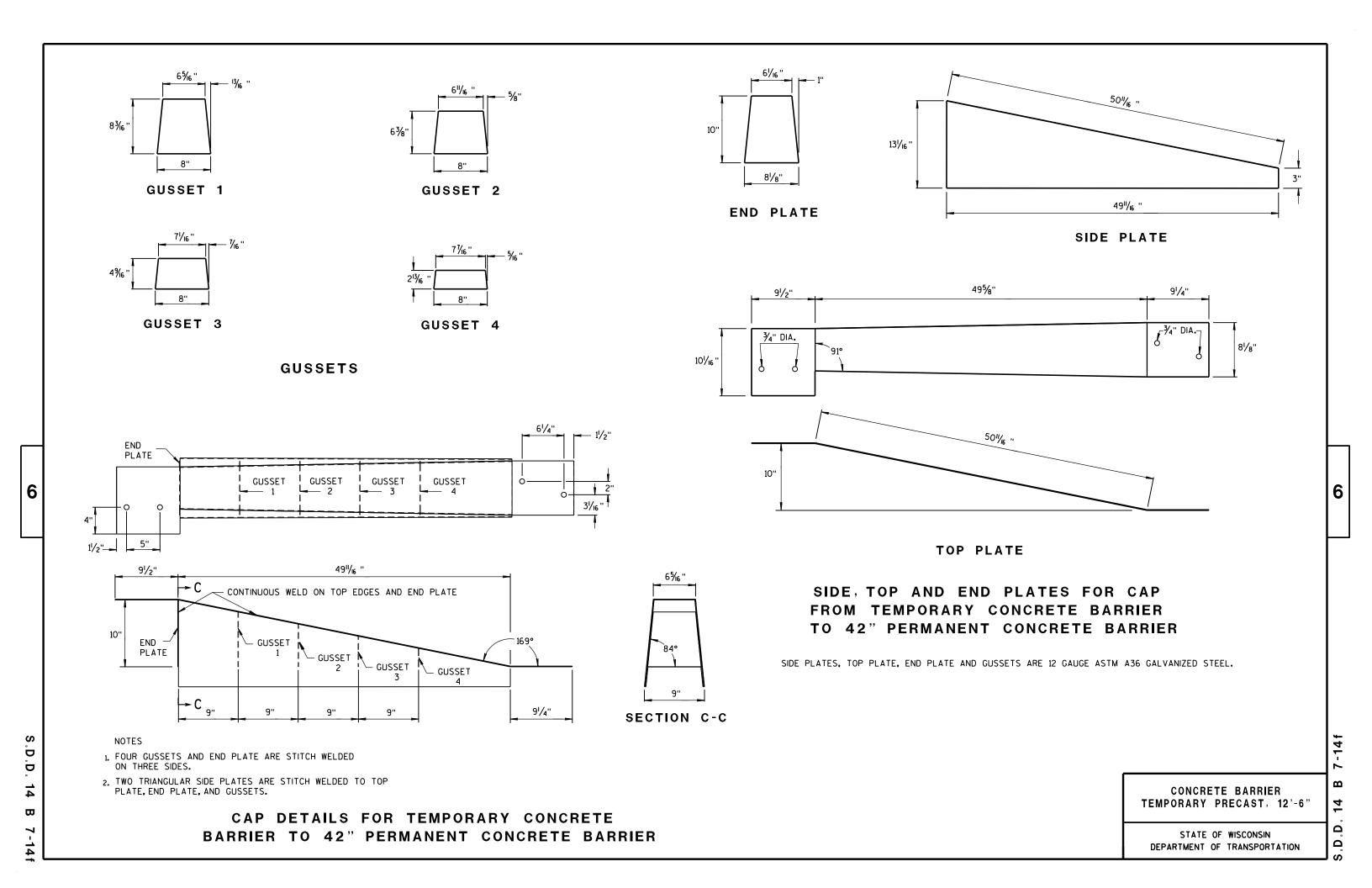
STOP PLATE

PLATE

DEPARTMENT OF TRANSPORTATION

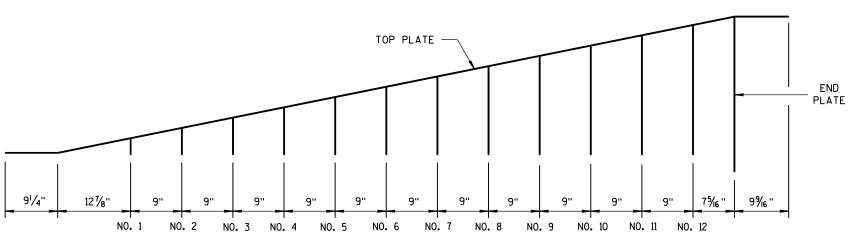
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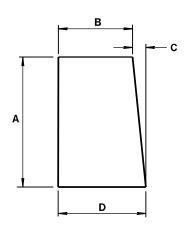
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GUSSET LOCATION

CAP DETAILS FOR TEMPORARY CONCRETE BARRIER TO 56" PERMANENT CONCRETE BARRIER



GUSSETS 1 - 12

ALL GUSSETS 1/8" STEEL PLATE

GUSSET DIMENSIONS					
GUSSET NO.	A	В	С	D	
1	21/8"	73/4"	1/4"	8	
2	4"/16 "	7% "	1/2"	8	
3	61/2"	73/8"	11/16 "	8½6"	
4	85%"	73/16"	⅓ "	81/16"	
5	101/8"	7"	1 1/16 "	81/16"	
6	11 ¹⁵ / ₁₆ ''	6 ¹³ // ₆ "	1 1/4"	81/16"	
7	13¾"	65/8"	1 1/6"	81/16 "	
8	15% "	6 ½ "	1 % "	81/16"	
9	173/8"	61/4"	1 13/16 ''	81/16"	
10	193/6"	6½ ₆ "	1 15/16 "	81/16 "	
11	21"	5 1/8"	23/6"	8½ ₆ "	
12	22 ¹³ / ₁₆ "	5 ¹¹ / ₁₆ "	25/6"	8½ ₆ "	

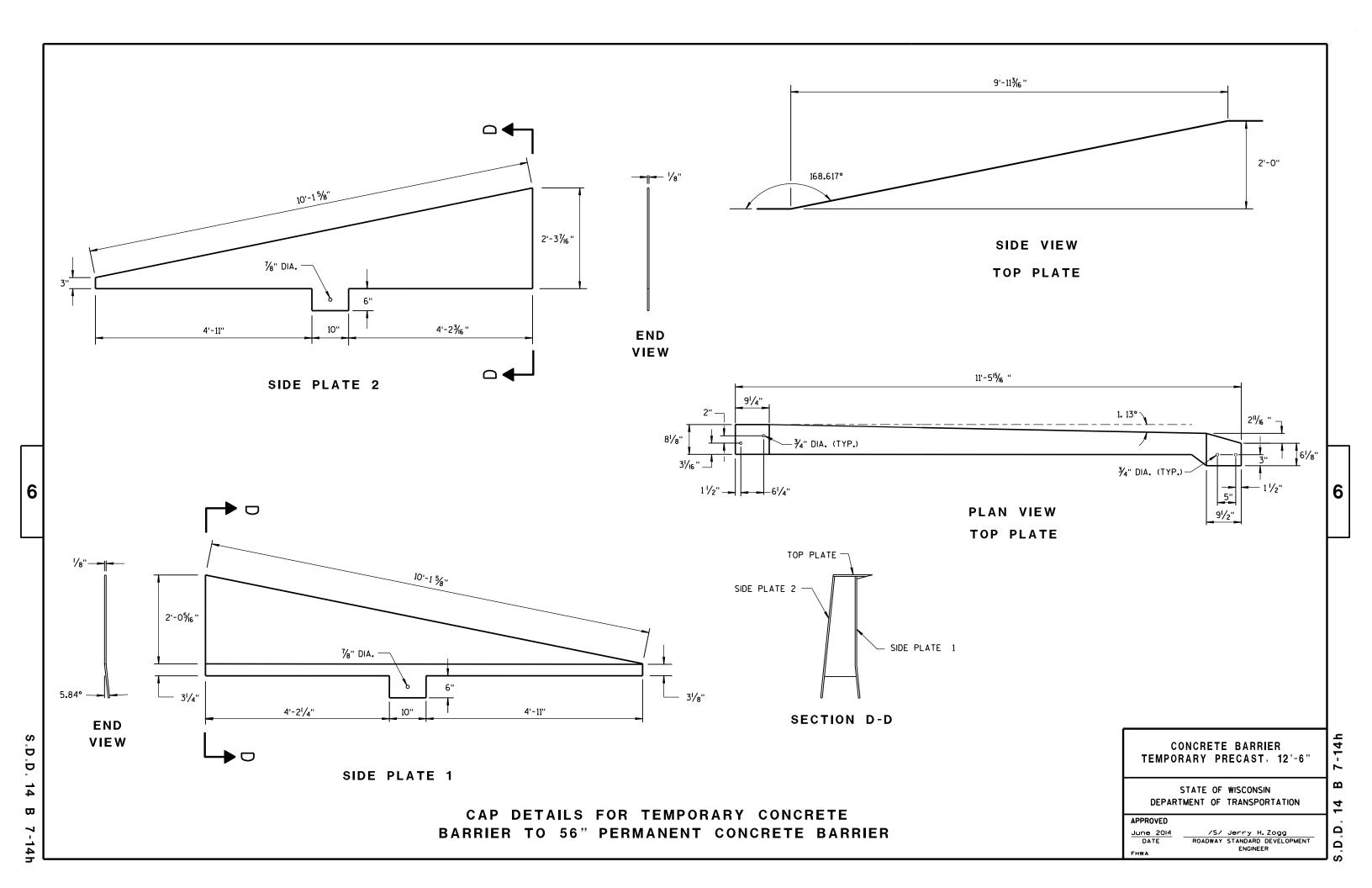
SIDE PLATES, TOP PLATE, END PLATE AND GUSSETS ARE 12 GAUGE ASTM A36 STEEL AND GALVANIZED.

GUSSETS AND END PLATE ARE STITCH WELDED ON 3 SIDES. TWO TRIANGULAR SIDE PLATES ARE STITCH WELDED TO TOP PLATE, END PLATE AND GUSSETS.

> CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"

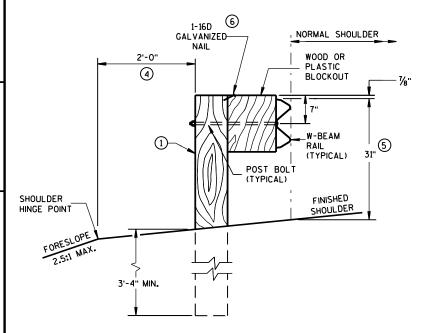
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

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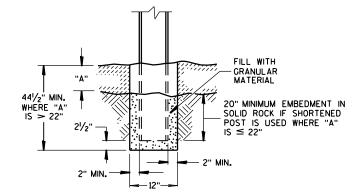
GENERAL NOTES

- (1) WOOD OR STEEL POSTS (W6X9 OR W6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- 2 USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- (3) IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 21/2 INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- (4) WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- (5) FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS ± 1". FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 273/4" TO 32".
- (6) WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.



END VIEW

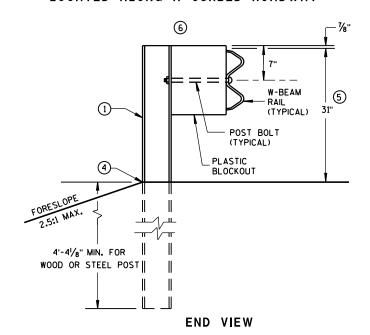
LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION



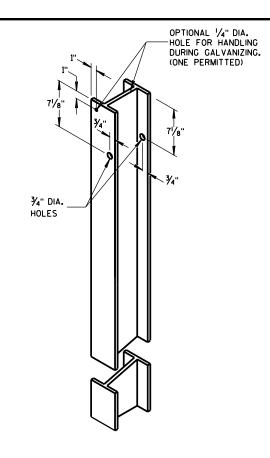
SETTING STEEL OR WOOD POST IN ROCK $^{\scriptsize{\textcircled{3}}}$



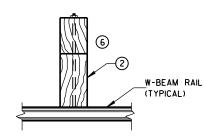
END VIEW
LOCATED ALONG A CURBED ROADWAY



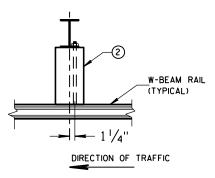
MGS LONGER POST AT HALFPOST SPACING W BEAM (K)



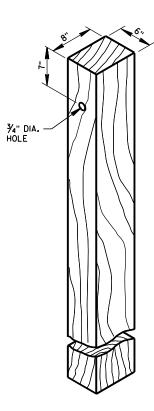
STEEL POST & HOLE PUNCHING DETAIL (w6X9)



PLAN VIEW
WOOD POST,
BLOCKOUT & BEAM



PLAN VIEW
STEEL POST,
PLASTIC BLOCKOUT & BEAM



WOOD POST (6" X 8") NOMINAL $^{\scriptsize \textcircled{1}}$



WOOD OR PLASTIC BLOCKOUT

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

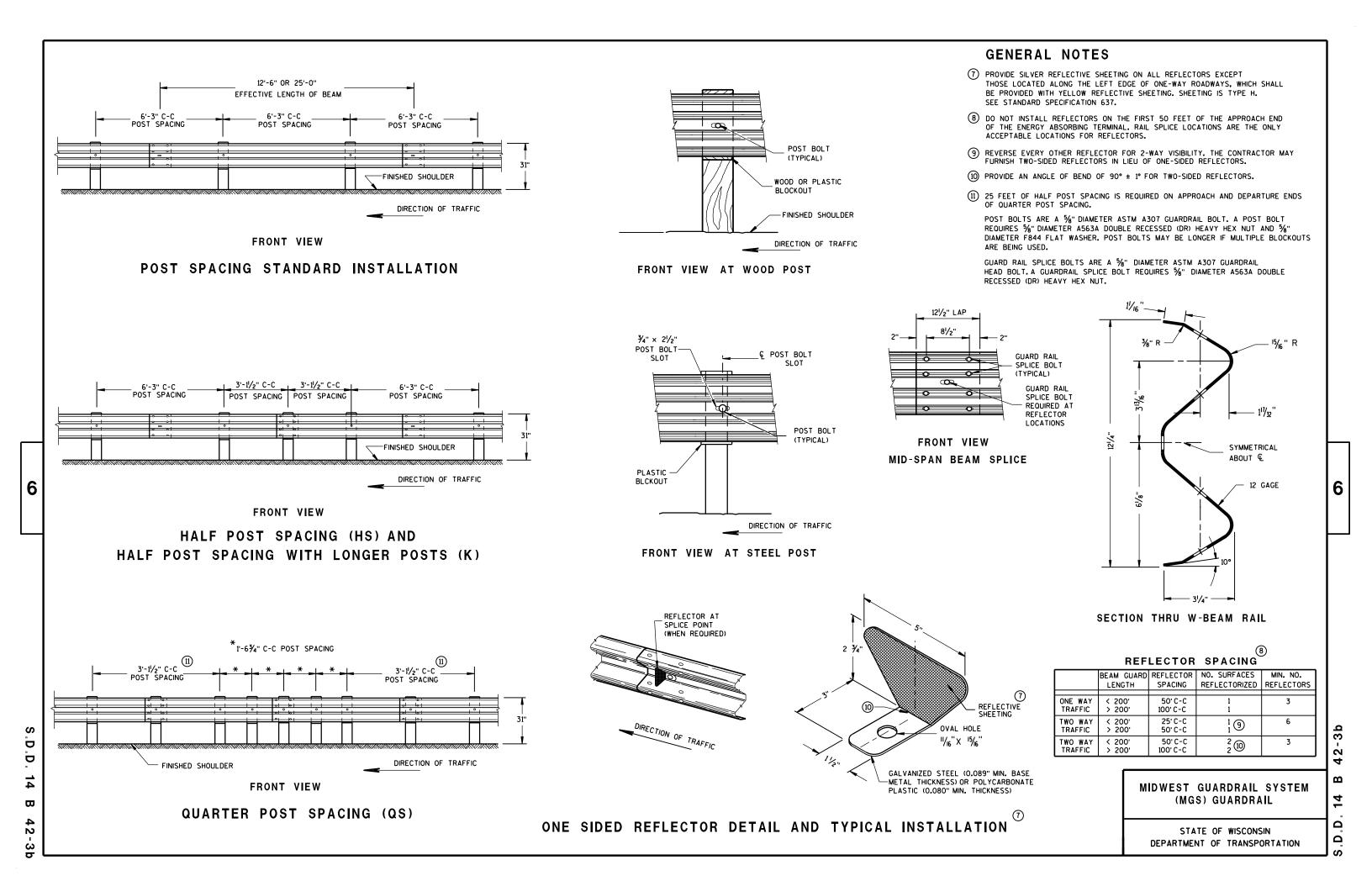
S.D.D. 14 B 4

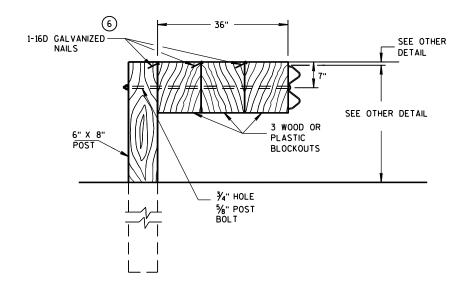
6

.D.D. 14 B

3a

2



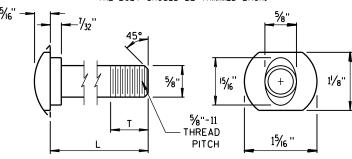


DETAIL FOR 36" BLOCKOUT DEPTH

NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL.

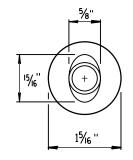
> DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

NOTE: 1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF $\frac{1}{16}$ ". 2. IF THE BOLT EXTENDS MORE THAN 1/4" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

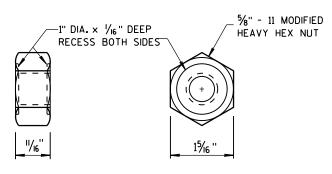


POST BOLT TABLE

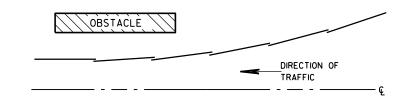
11/8"
437
13/4"
4"
41/16"
4"
41/16"
4"



ALTERNATE BOLT HEAD

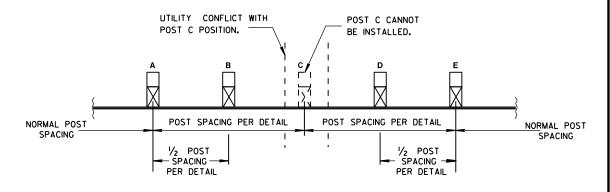


POST BOLT AND RECESS NUT



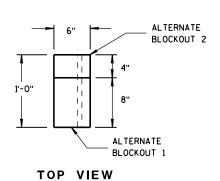
PLAN VIEW

BEAM LAPPING DETAIL



POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION





SIDE VIEW

ALTERNATE WOOD **BLOCKOUT DETAIL**

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED

June 2014 /S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER FHWA

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S.D.D.

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SECTION A-A SECTION B-B

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PLAN VIEW

BILL OF MATERIALS

PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.
1	WOOD BREAKAWAY POST
2	6" X 8" X 0.188", 6'-0" LONG FOUNDATION TUBE AT POSTS 1AND 2
3	WOOD CRT
4	WOOD BLOCKOUT
(5)	PIPE SLEEVE
6	BEARING PLATE
7	BCT CABLE ASSEMBLY
8	ANCHOR CABLE BOX
9	GROUND STRUT
10	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
(11)	STANDARD W-BEAM RAIL.MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
12	END SECTION EAT
(3)	0.040" ALUMINUM SHEET WITH REFLECTIVE SHEETING TYPE F PER SECTION 637 OF THE STANDARD SPECIFICATIONS
14)	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)



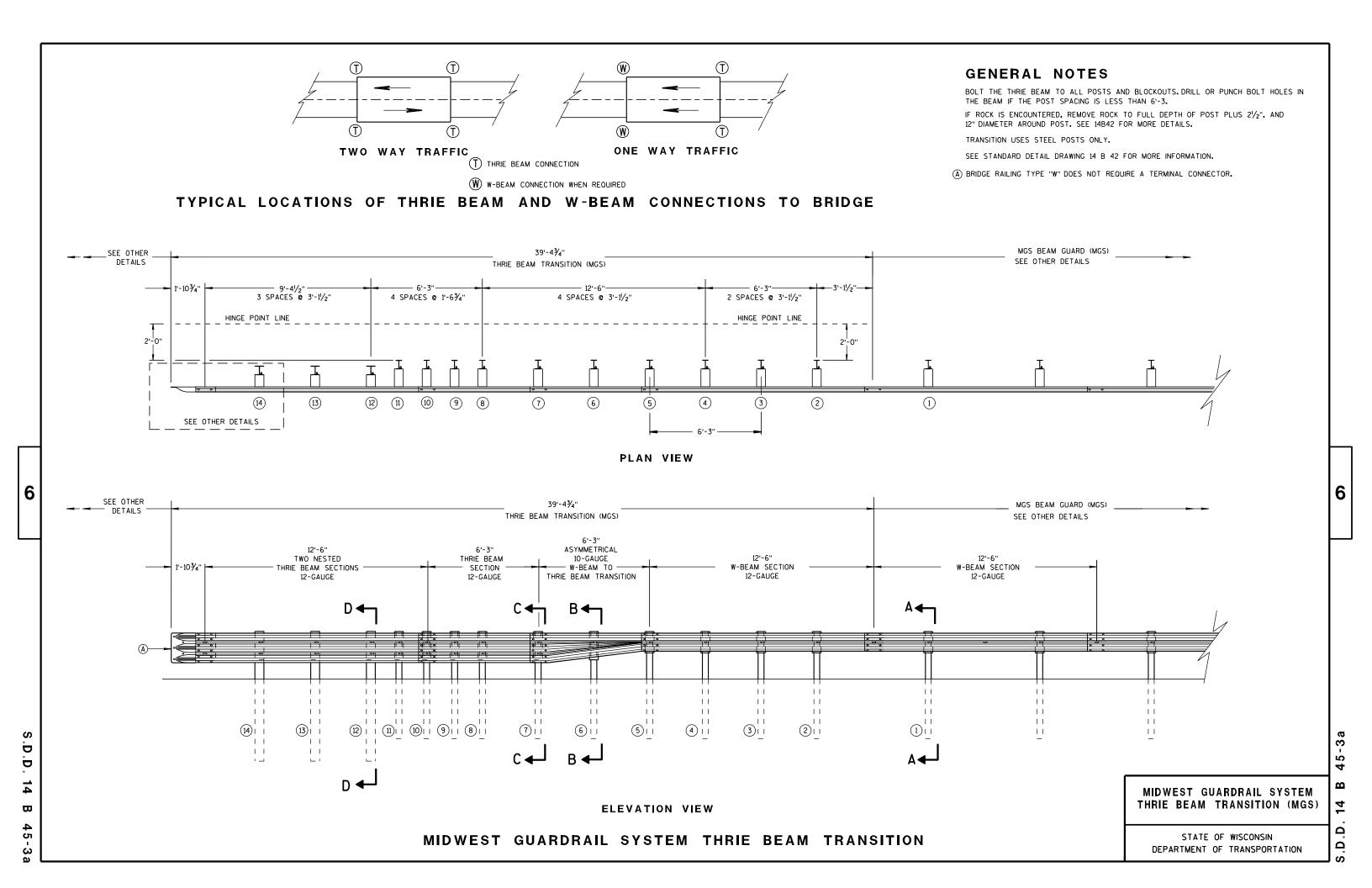
MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)

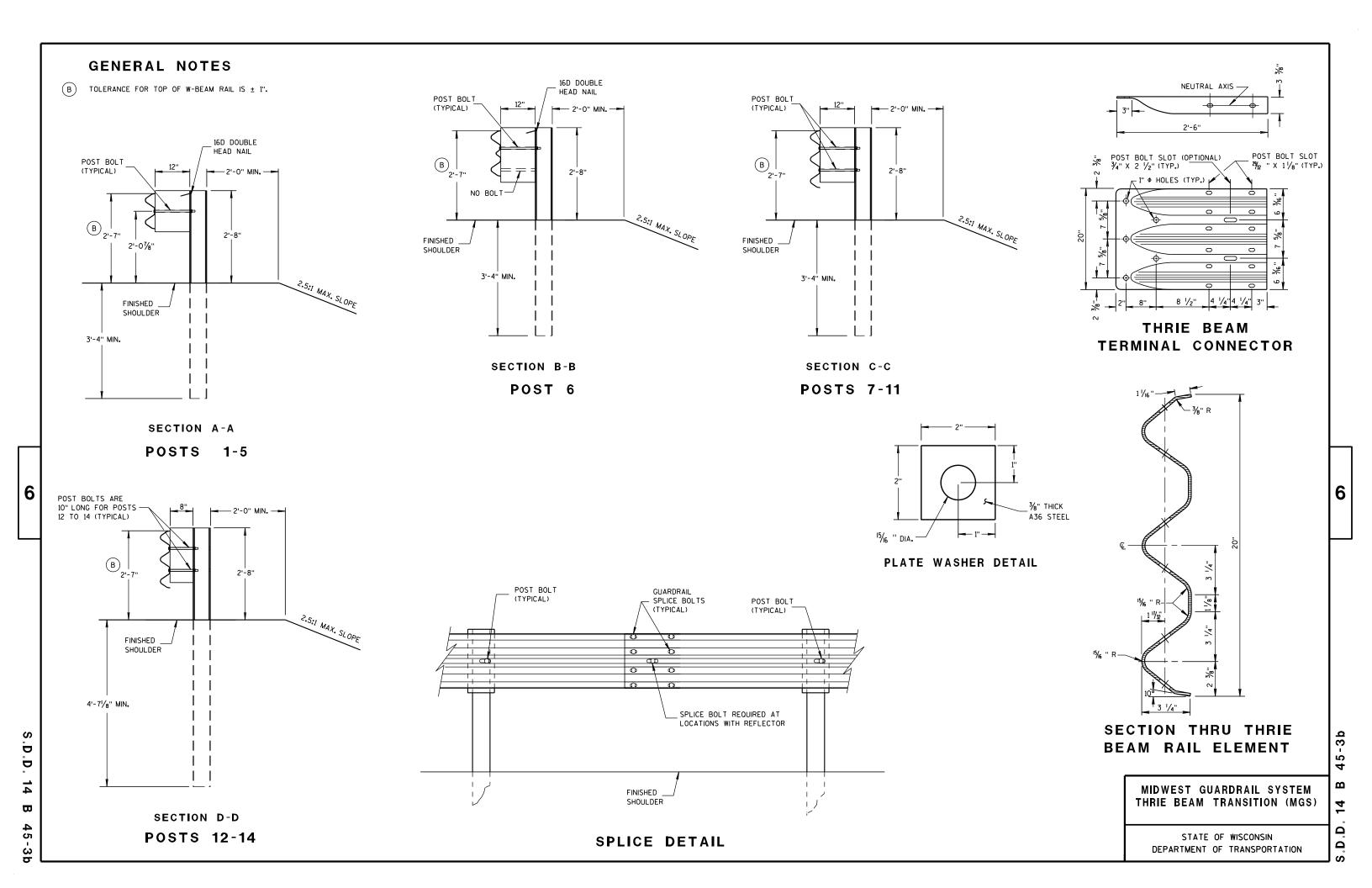
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

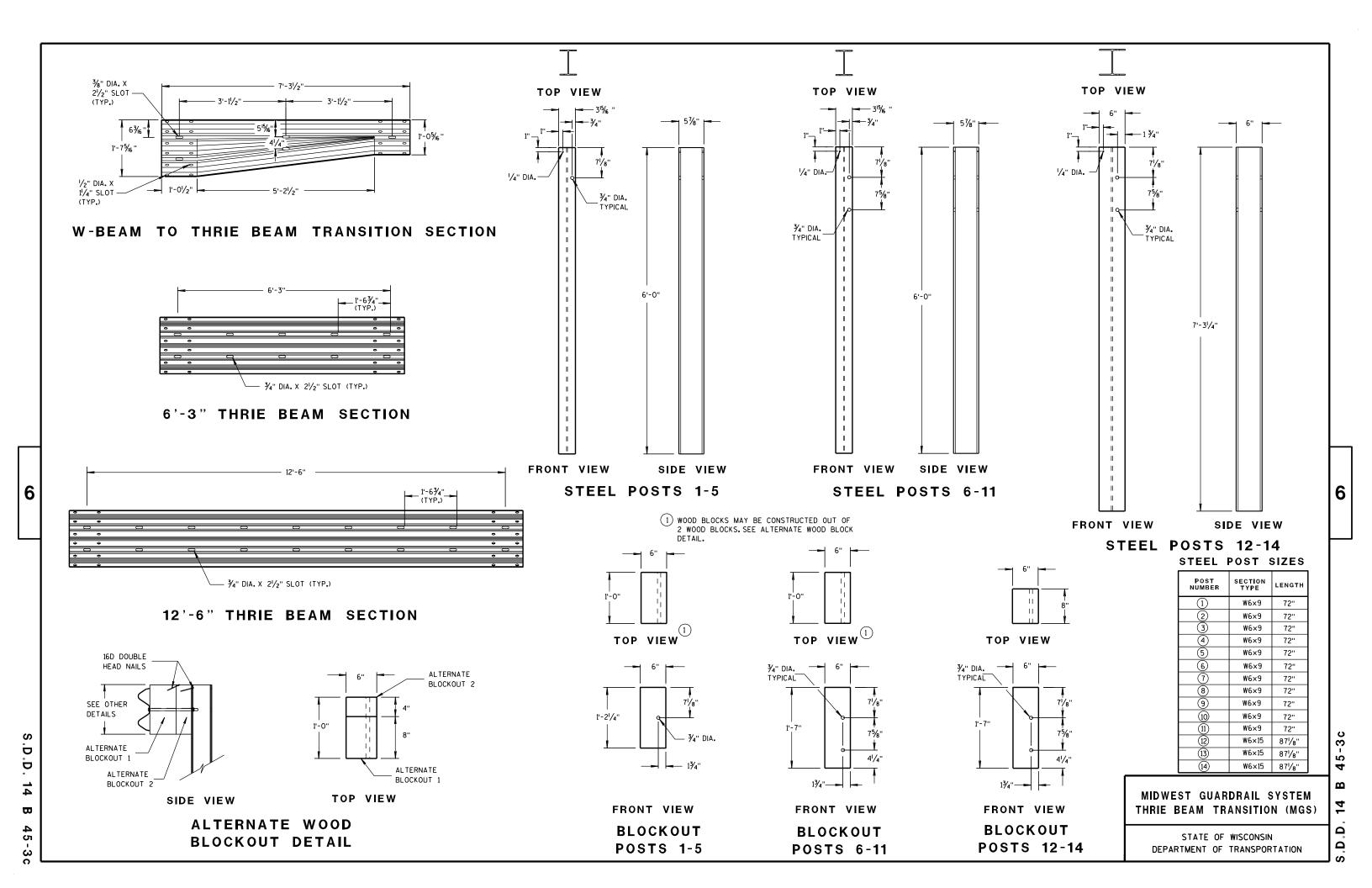
44-2b

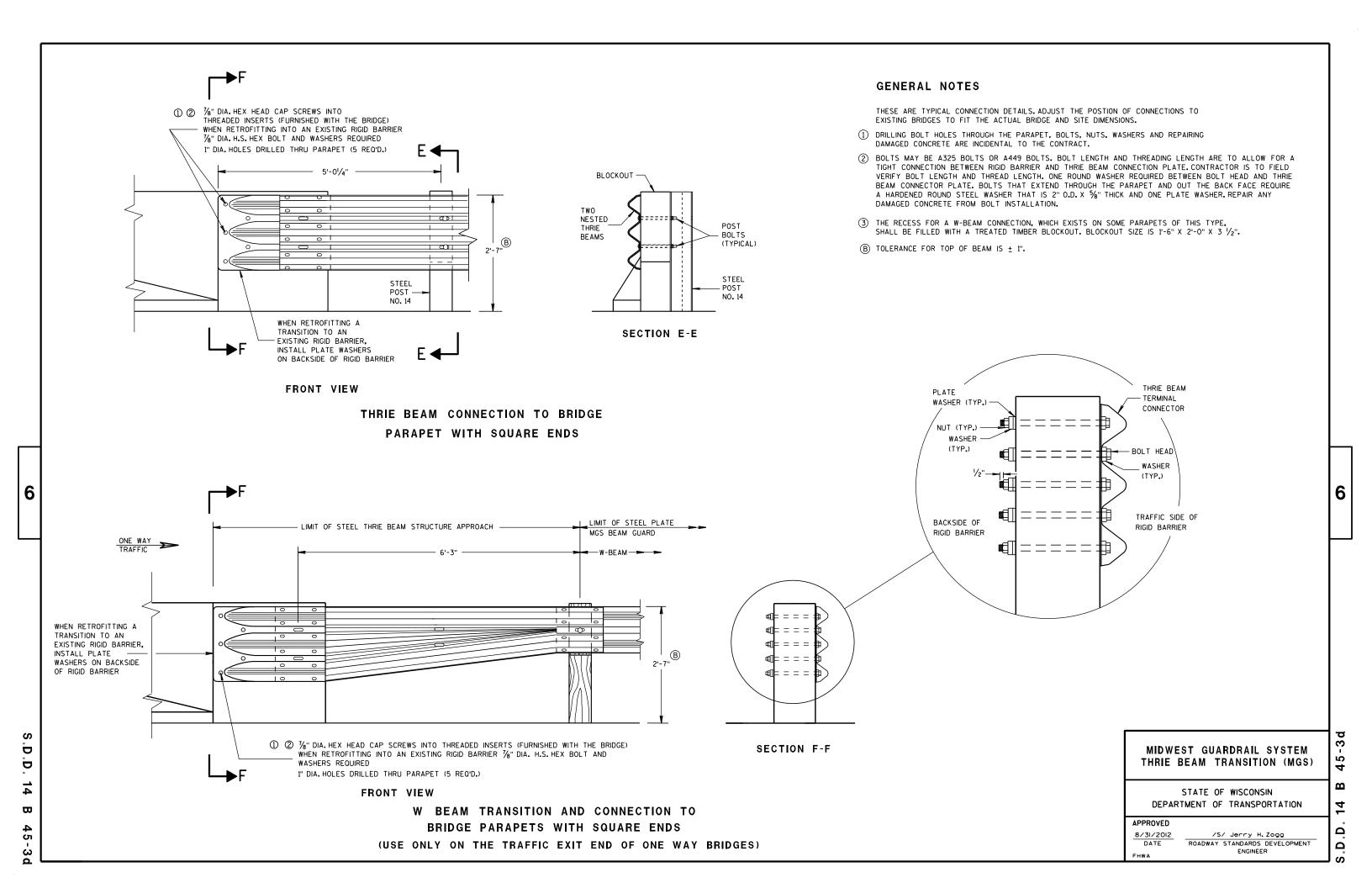
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	CONNE		R ASSEMBLY)	ION
PLATE	QUANTITY	SHAPE	SIZE (A × B × C × D)	THICKNESS
P1	1	в₫	20" × 20"	3/6 "
P2	1	B∱c	20" × 20" × 28%6"	¾6 "
Р3	1	B C D	39" × 35/8" × 20" × 191/6"	3∕16 ''
S1	4	B	18 1/6 " × 3 1/8" × 18 3/4"	1/4"
S2	1	B C D	10 ¹ / ₄ " × 2 ¹ / ₁₆ " × 10 ³ / ₈ " × ¹ / ₂ "	1/4"
S3	1	B C D	$3" \times 1^{1}/_{16}" \times 3^{1}/_{8}" \times 1^{1}/_{2}"$	1/4"
S4	1	вД	6½" × 2½6"	1/4"
S5	1	В	6½" × ½"	1/4"
S6	1	В	7¾" × 1¾"	1/4"
S7	1	A DC	2%6" × 6" × 3%" × 5%"	1/4"
S8	1	A∰C	1 ⁵ / ₃₂ " × 7 ¹ / ₂ " × 2 ¹ / ₂ " × 7 ³ / ₈ "	1/4"
S9	1	C 	$6\frac{1}{16}$ " × $6\frac{3}{16}$ " × $1\frac{3}{32}$ "	1/4"
S10	1	A D C	1%" × 9%" × 3%" × 911/16"	1/4"
S11	1	C A	8½" × 8¾" × 1⅓6 "	1/4"

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SINGLE SLOPE CONNECTION PLATE

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

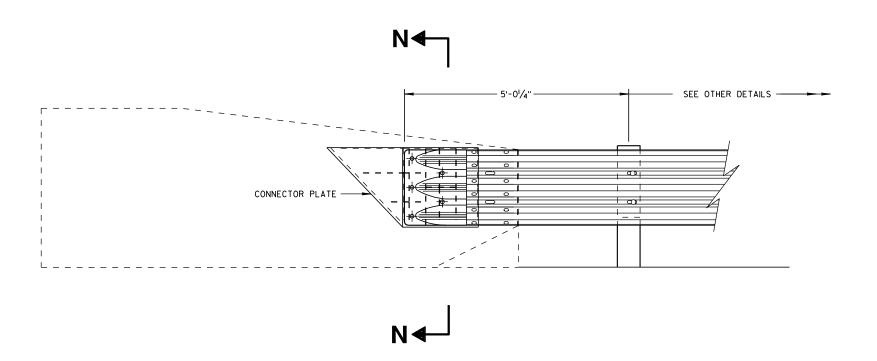
8/31/2012 /S/ Jerry H. Zogg

DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

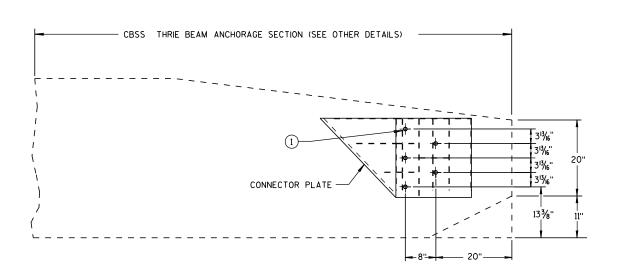
S.D.D. 1

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THRIE BEAM CONNECTION TO SINGLE SLOPE BARRIER

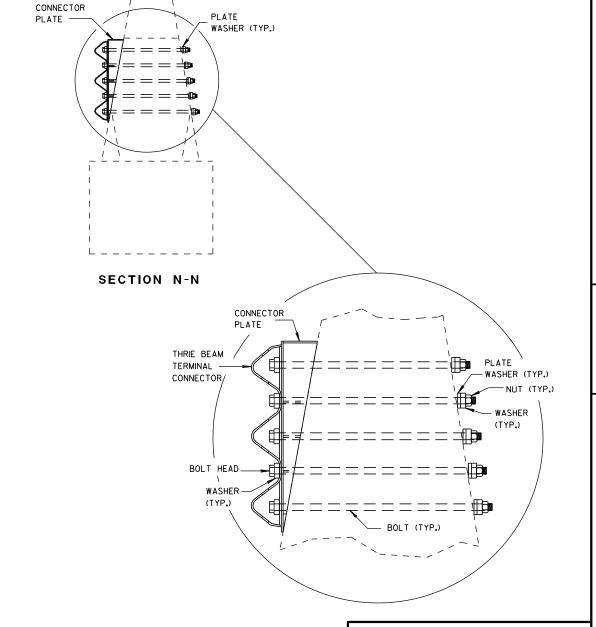


SINGLE SLOPE CONNECTION PLATE PLACEMENT

GENERAL NOTES

CONNECTOR PLATE, DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/8" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.



MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)

45

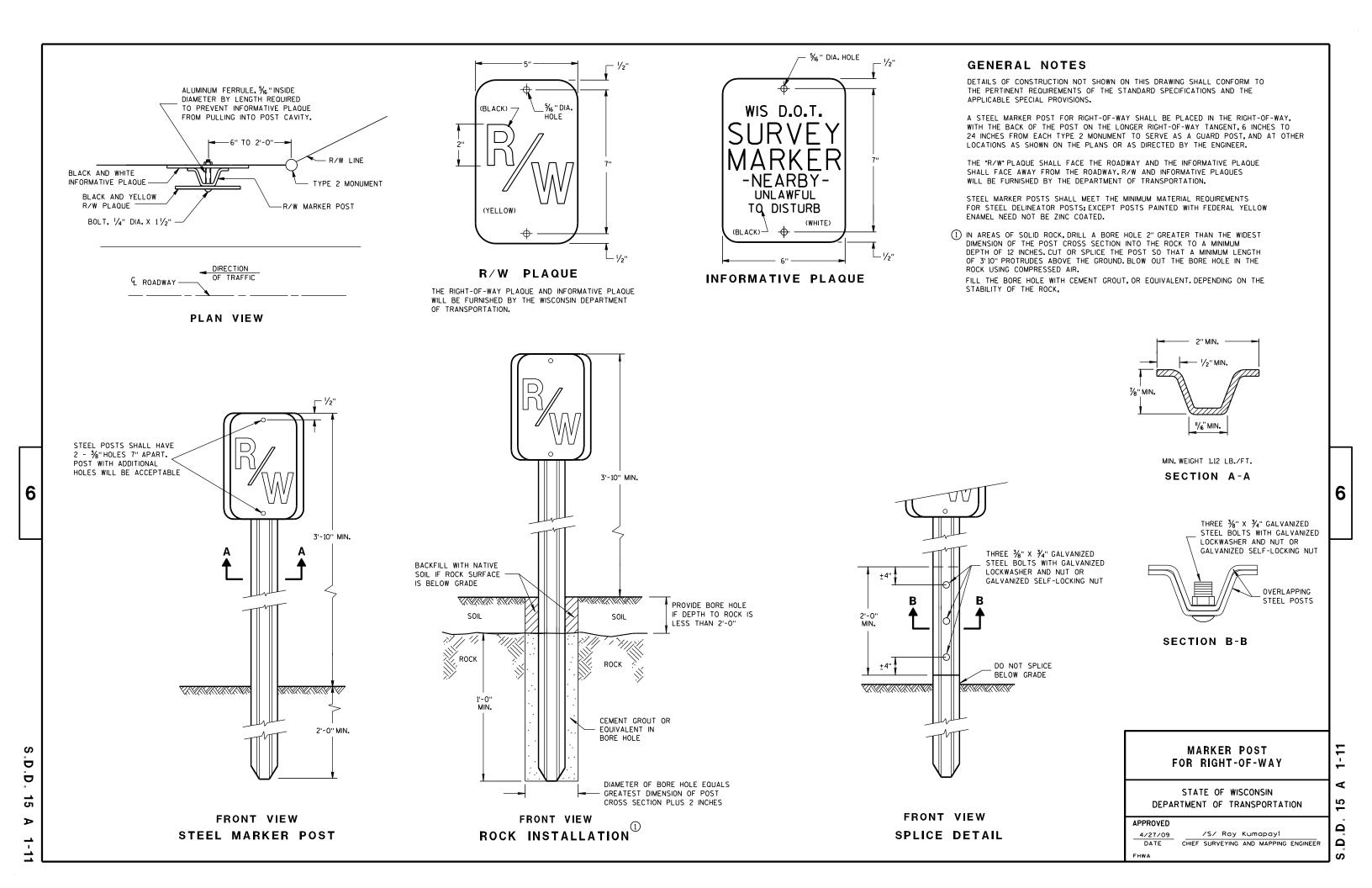
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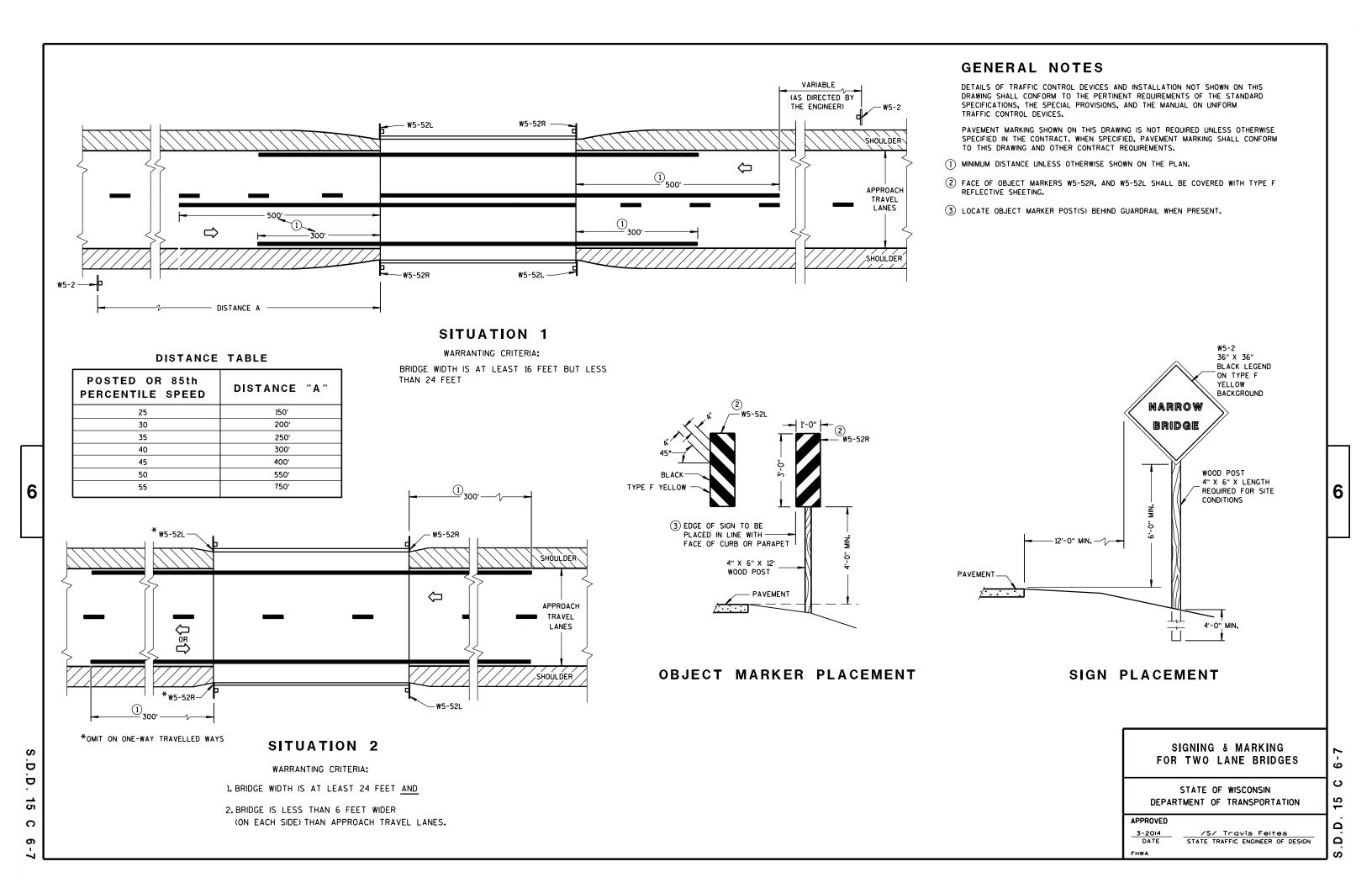
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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

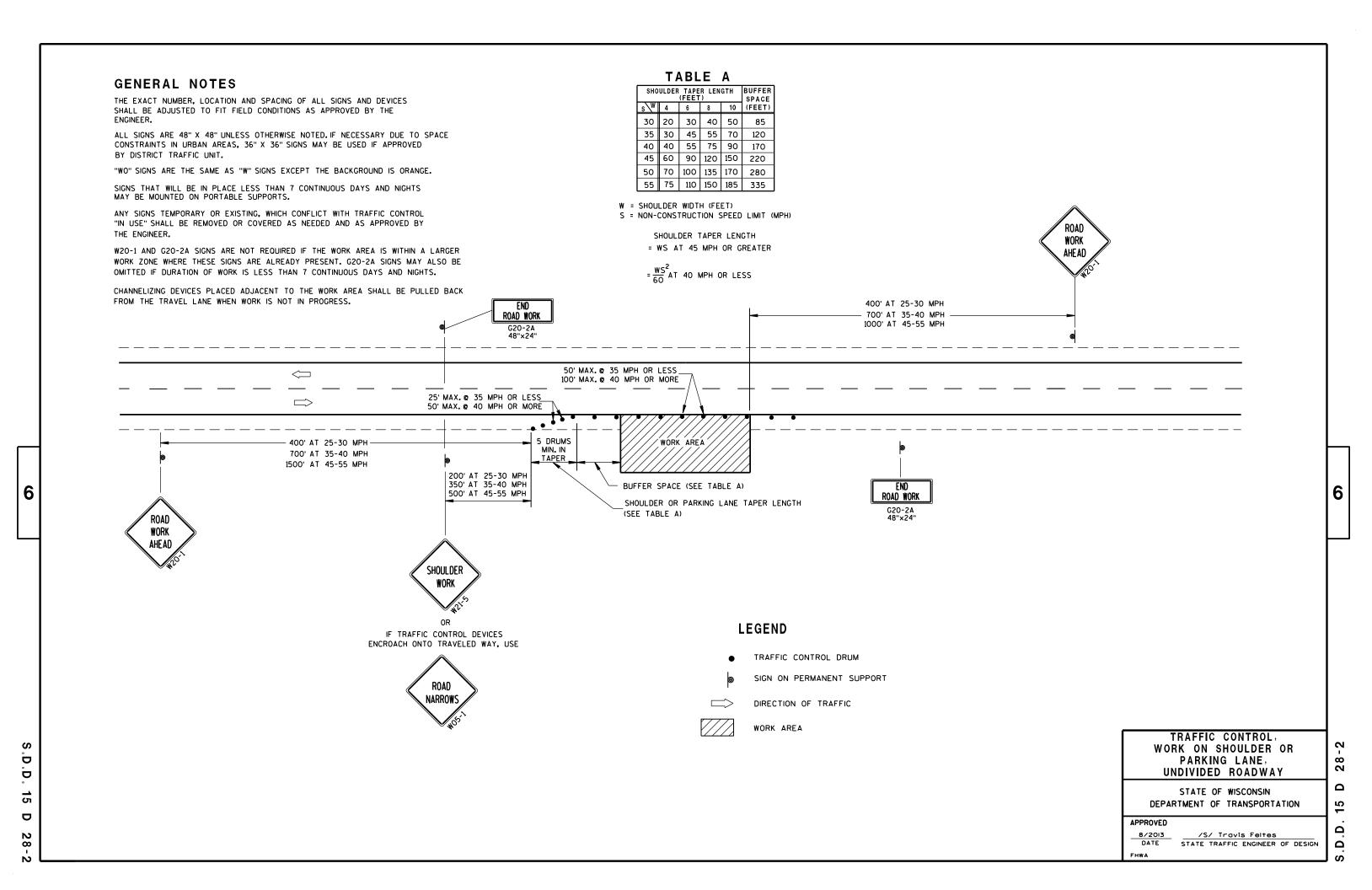
APPROVED 8/31/2012

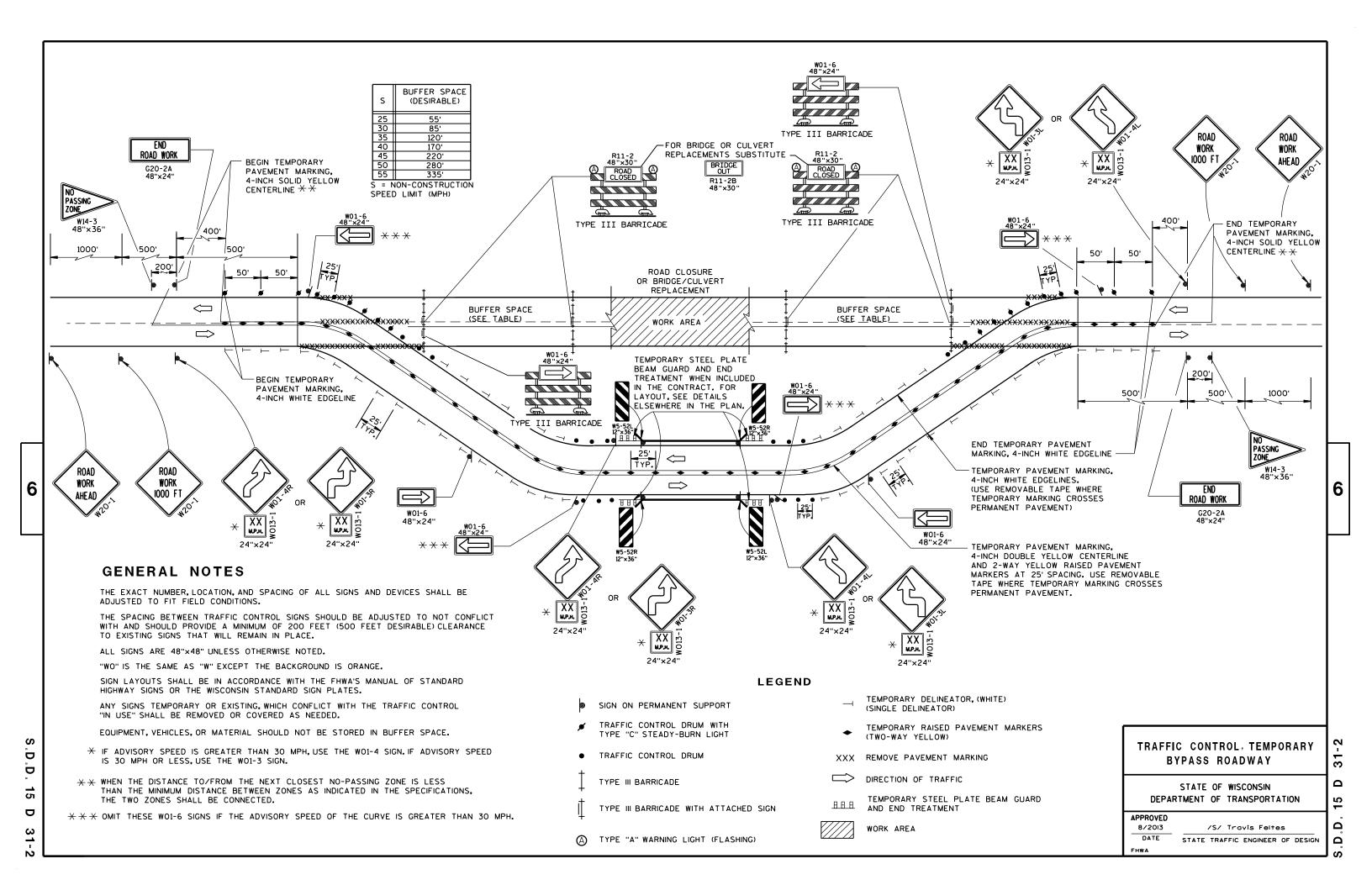
/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER



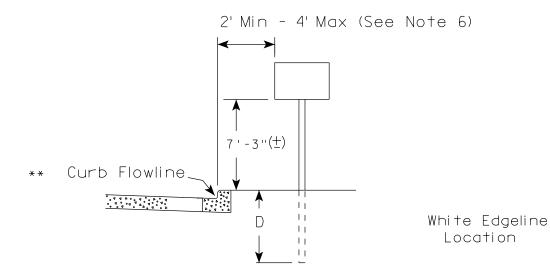




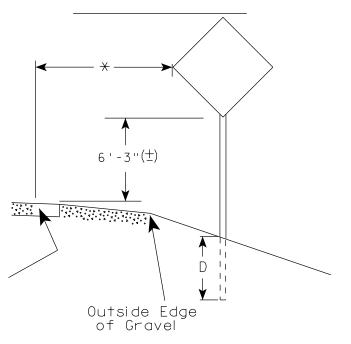




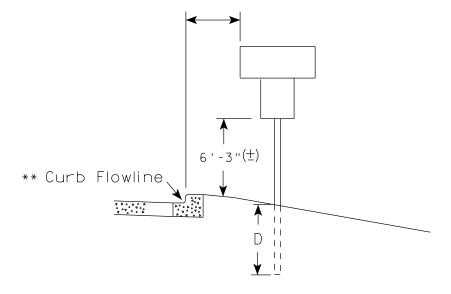
urban area



RURAL AREA (See Note 2)



2' Min - 4' Max (See Note 6)



White Edgeline
Location

Outside Edge
of Gravel

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated.

That height is typically measured where

there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

HWY:

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

PLOT BY : mscj9h

GENERAL NOTES

- 1. Signs wider than 4 feet, 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
- 2. If signs are mounted on barrier wall, see A4-10 sign plate.
- 3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
- 4. Minimum mounting height for J assemblies (A4-5) is 7'-3'' (\pm) or 6'-3'' (\pm) per urban or rural detail respectively.
- 5. Minimum mounting height for signs mounted on traffic signal poles is 5' 3" (\pm) .
- 6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
- 7. The (\pm) tolerance for mounting height is 3 inches.
- 8. Folding stop signs (R1-1F) shall be mounted at a height of 5'-3" (\pm) or as directed by the Engineer.
- 9. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series) & End of Rod Markers (W5-56 & W5-56A) shall be mounted at a height of 4'-3" (+).

POST EMBEDMENT DEPTH

Area of Sign	
Installation	D
(Sq. Ft.)	(Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matther & Rauh
For State Traffic Engineer

DATE 9/30/13

SHEET NO:

COUNTY:

JN I Y:

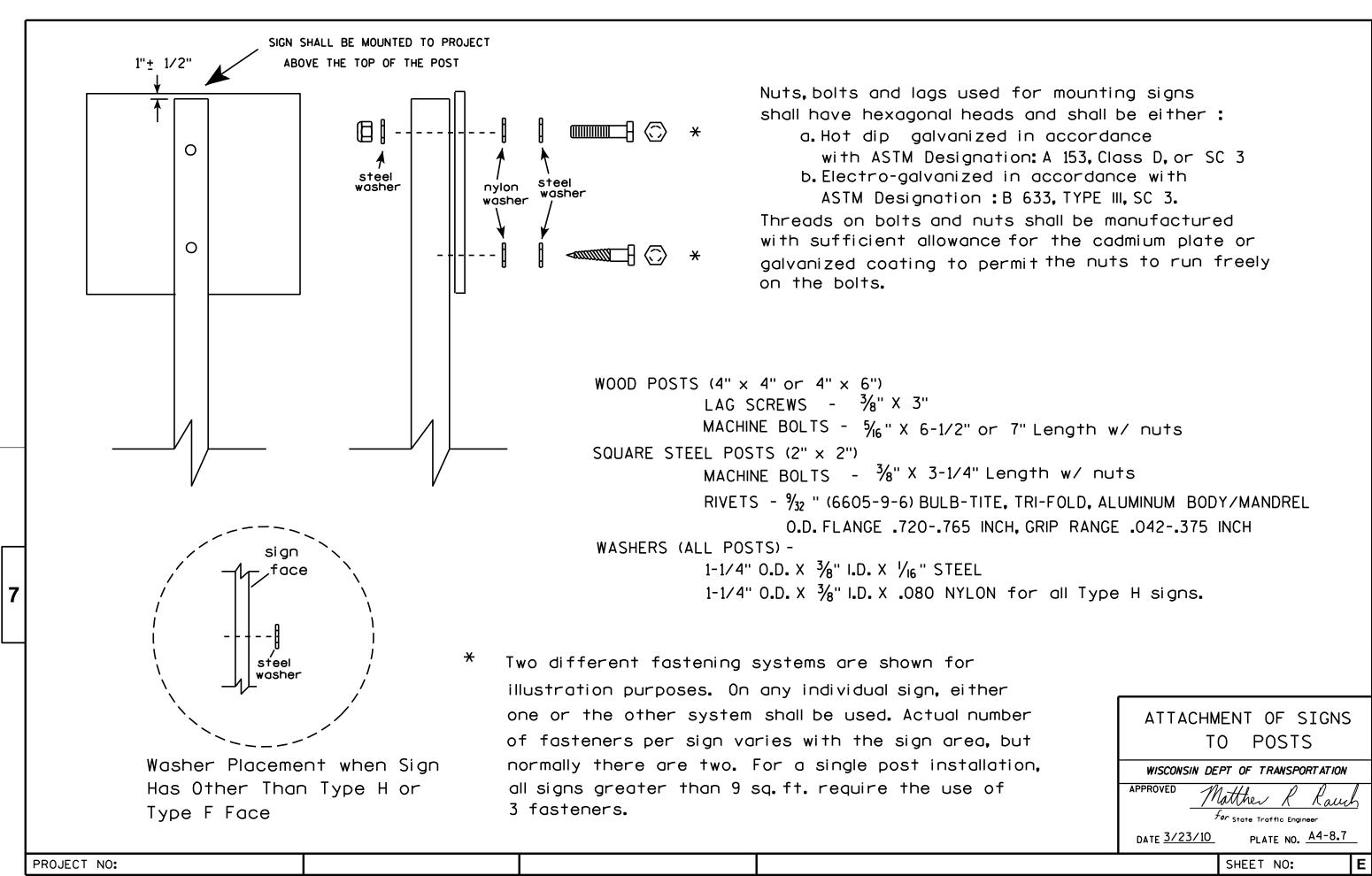
PLOT DATE: 30-SEP-2013 13:25

PLOT NAME :

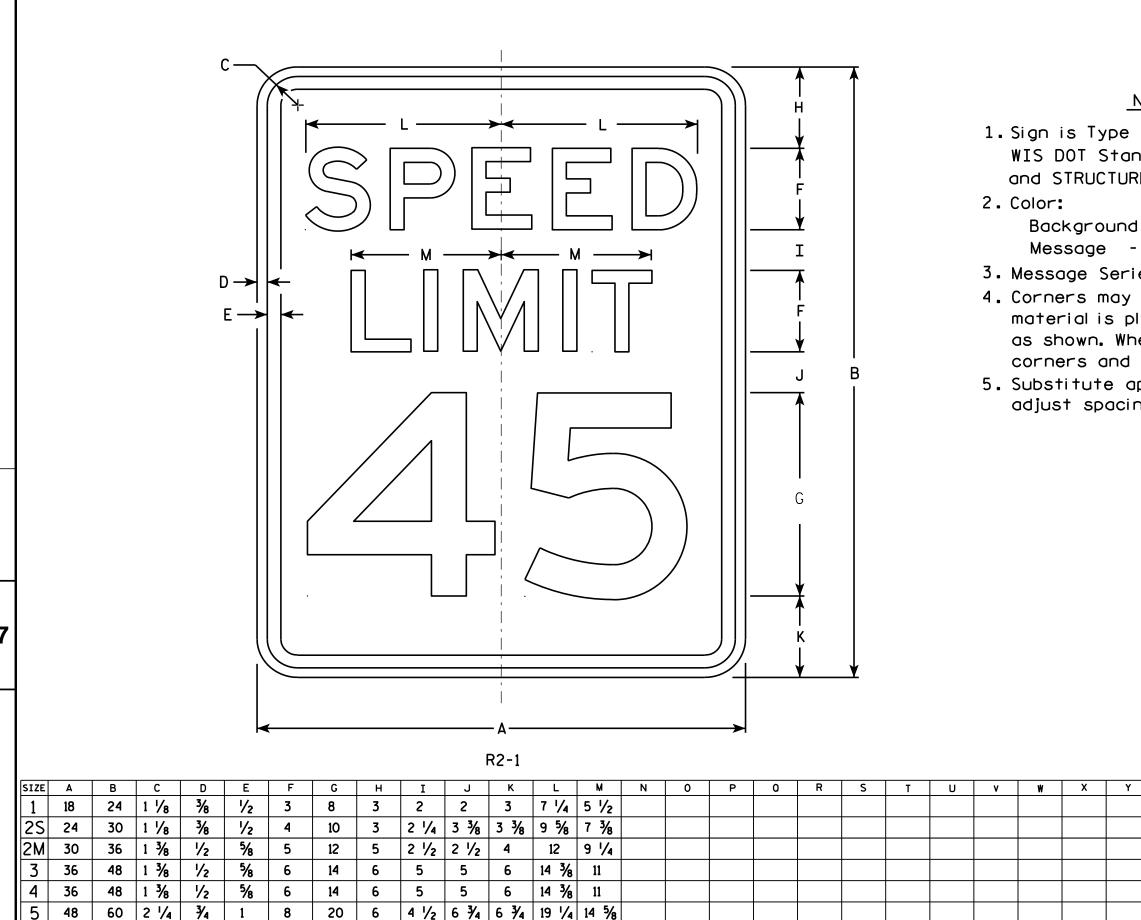
PLOT SCALE: 99.237937:1.000000

WISDOT/CADDS SHEET 42

PROJECT NO:







COUNTY:

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.

Background - White Message - Black

- 3. Message Series E
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal. the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

3.0

5.0

7.5

12.0

12.0

20.0

STANDARD SIGN R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matther R Raus

For State Traffic Engineer DATE <u>5/26/1</u>0 PLATE NO. R2-1.13

SHEET NO:

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R21.DGN

PROJECT NO:

HWY:

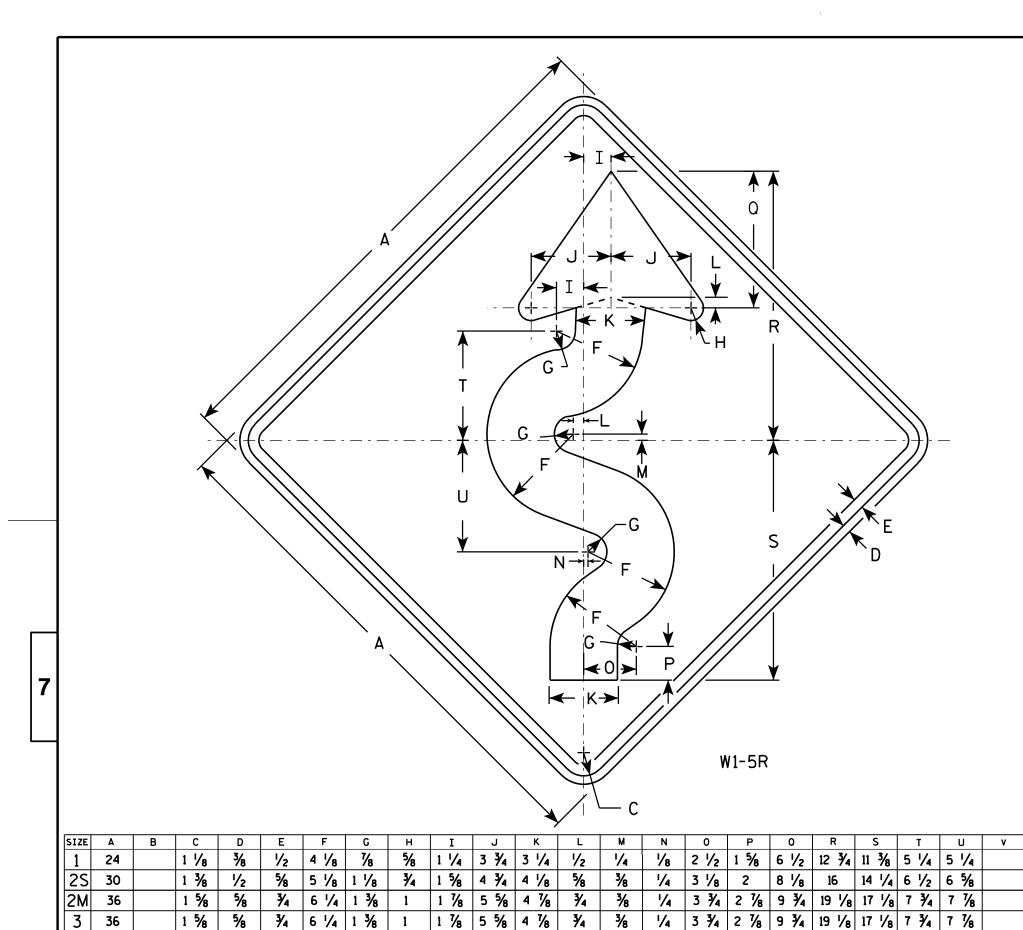
PLOT DATE: 28-MAY-2010 08:32

PLOT BY : ditjph

PLOT NAME :

PLOT SCALE: 4.717577:1.000000

WISDOT/CADDS SHEET 42



1 1/8 | 5 5/8 | 4 1/8 | 3/4

3/8

COUNTY:

1/2

8 1/4 1 3/4 1 1/4 2 1/2 7 1/2 6 1/2 1

HWY:

NOTES

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 4. W1-5L is the same as W1-5R except the arrow is reversed along the vertical centerline.

STANDARD SIGN
W1-5
WISCONSIN DEPT OF TRANSPORTATION

APPROVED M

DATE 5/18/12

3/12 PLATE NO. W1-5.8

PLOT DATE : 18-MAY-2012 13:08 PLOT B

| 25 1/2 | 22 3/4 | 10 3/8 | 10 1/2 |

3 3/4 2 1/8 9 3/4 19 1/8 17 1/8 7 3/4 7 1/8

PLOT NAME :

4.0

6.25

9.0

9.0

9.0

16.0

SHEET NO:

1 %

2 1/4

4

5

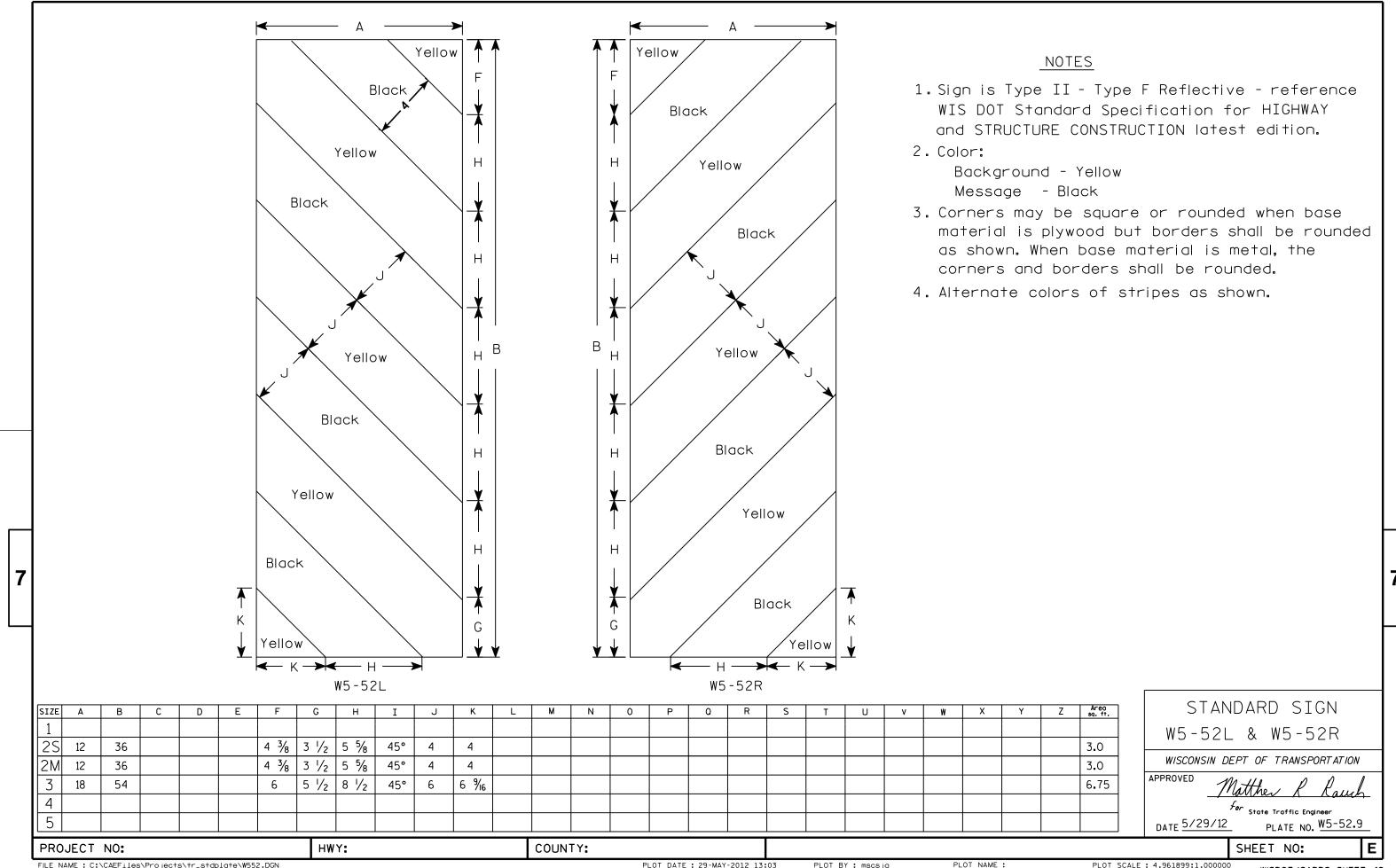
36

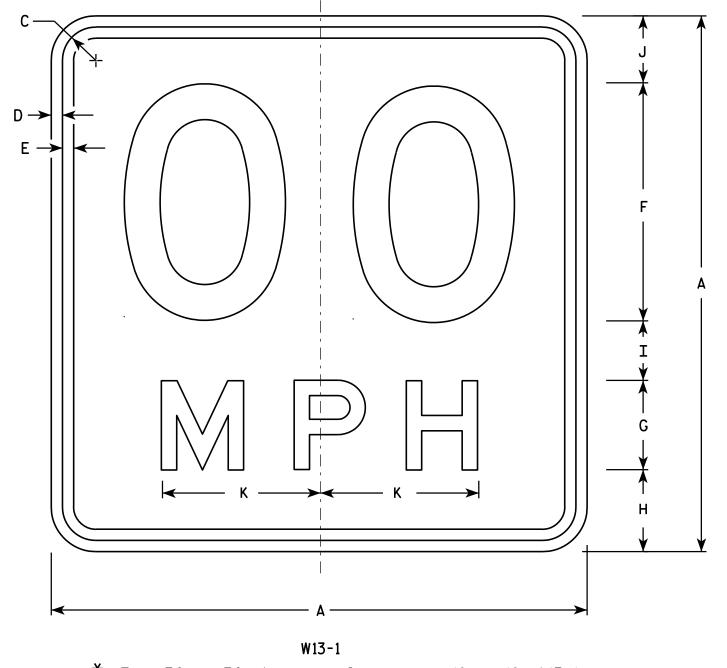
48

PROJECT NO:

5/8

3/4 | 6 | /₄ | 1 3/₈





NOTES

- 1. Sign is Type II Type F Reflective reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
- 2. Color:

Background - Yellow Message - Black

- 3. Message Series See Note 6
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
- 6. Line 1 is Series D Line 2 is Series E

* For 30" \times 30" Warning Signs, use 18" \times 18" W13-1 signs. For 36" \times 36" Warning Signs, use 24" \times 24" W13-1 signs.

SIZE	Α	В	С	D	E	F	G	Н	I	J	K	L	М	N	0	Р	0	R	S	T	U	٧	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 %																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 1/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 %																9.00
5	36		1 %	5/8	3/4	16	6	5 1/2	4	4 1/2	10 %																9.00

STANDARD SIGN W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew N

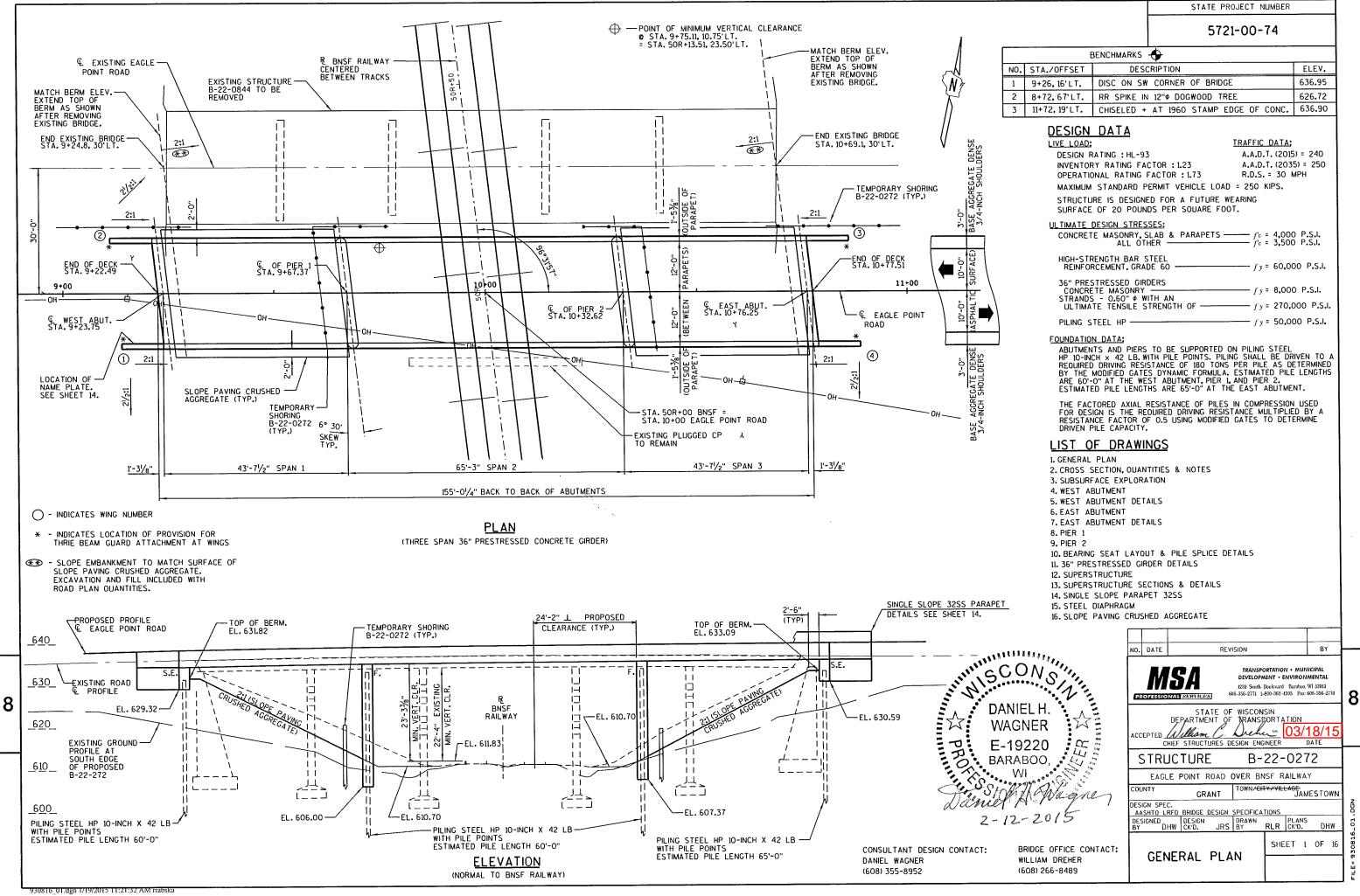
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

SHEET NO:

PLOT BY: mscsja

PLOT NAME :



GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.

THE FIRST DIGIT OF A THREE DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH SLOPE PAVING CRUSHED AGGREGATE TO THE LIMITS SHOWN ON SHEET 1 AND ON THE ABUTMENT SHEETS OR AS DIRECTED BY THE ENGINEER.

THE EXISTING GROUNDLINE SHALL BE THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES" FOR THE ABUTMENTS. CONSTRUCT ABUTMENTS ON ROAD FILL. FILL TO BE MEASURED AND PAID FOR SEPARATELY UNDER THE ROAD PORTION OF THE PROJECT.

THIS STRUCTURE WILL REPLACE EXISTING BRIDGE, B-22-0844, A 143.9 FOOT LONG, FIVE SPAN, CONCRETE DECK GIRDER BRIDGE SET ON OPEN FRAME CONCRETE ABUTMENTS AND OPEN FRAME CONCRETE PIERS.

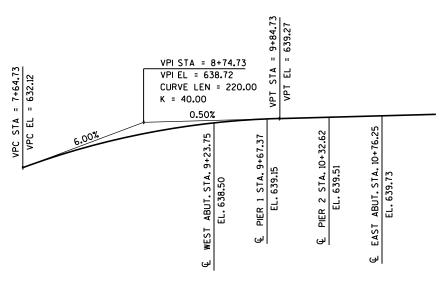
AT THE ABUTMENTS ALL EXCAVATED VOLUME NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH BACKFILL STRUCTURE. THE BACKFILL STRUCTURE ESTIMATED QUANTITIES ASSUMED A $1\frac{1}{2}$: 1 EXCAVATION SLOPE AT THE ABUTMENTS.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO USGS NAVD 88 DATA, A DISK ON THE SOUTHWEST BRIDGE CORNER (V110 1934), EL. 636.95.

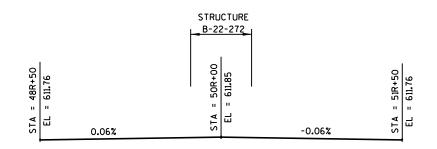
PROTECTIVE SURFACE TREATMENT SHALL BE APPLIED TO THE TOP OF SLAB AND TO THE INSIDE FACES AND TOPS OF THE PARAPETS.

THE 2 EXISTING BNSF TRACKS ARE SPACED APPROXIMATELY 14'-0" APART. THE PLAN LAYOUT ALLOWS FOR FUTURE TOTAL OF 3 TRACKS SPACED AT 15'-0" AND CENTERED ON THE $^{\rm R}$ OF THE BNSF RAILWAY.

CONCRETE PARAPET ON ABUTMENT WINGWALLS IS INCLUDED IN THE SUPERSTRUCTURE CONCRETE VOLUME.



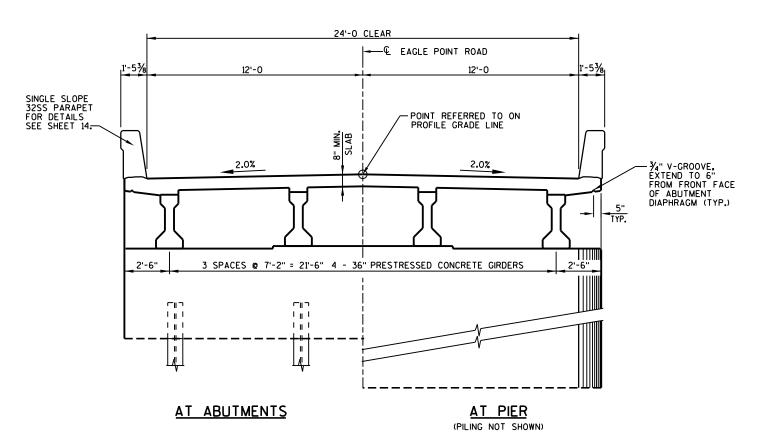
PROFILE GRADE LINE - EAGLE POINT ROAD



PROFILE GRADE LINE - BNSF RR

WESI	INACK	CONTROLS	

NO.	DATE	F	REVISION				В	Y
	[STATE DEPARTMENT (OF WISC OF TRAN			ION		
5	STRL	ICTURE	E	3-2	22-	027	2	
			DRAWN BY	RLF	₹	PLANS CK'D.	DH	W
	Q	SS SECTUANTITIES NOTES	S		SHE	ET 2	OF	16

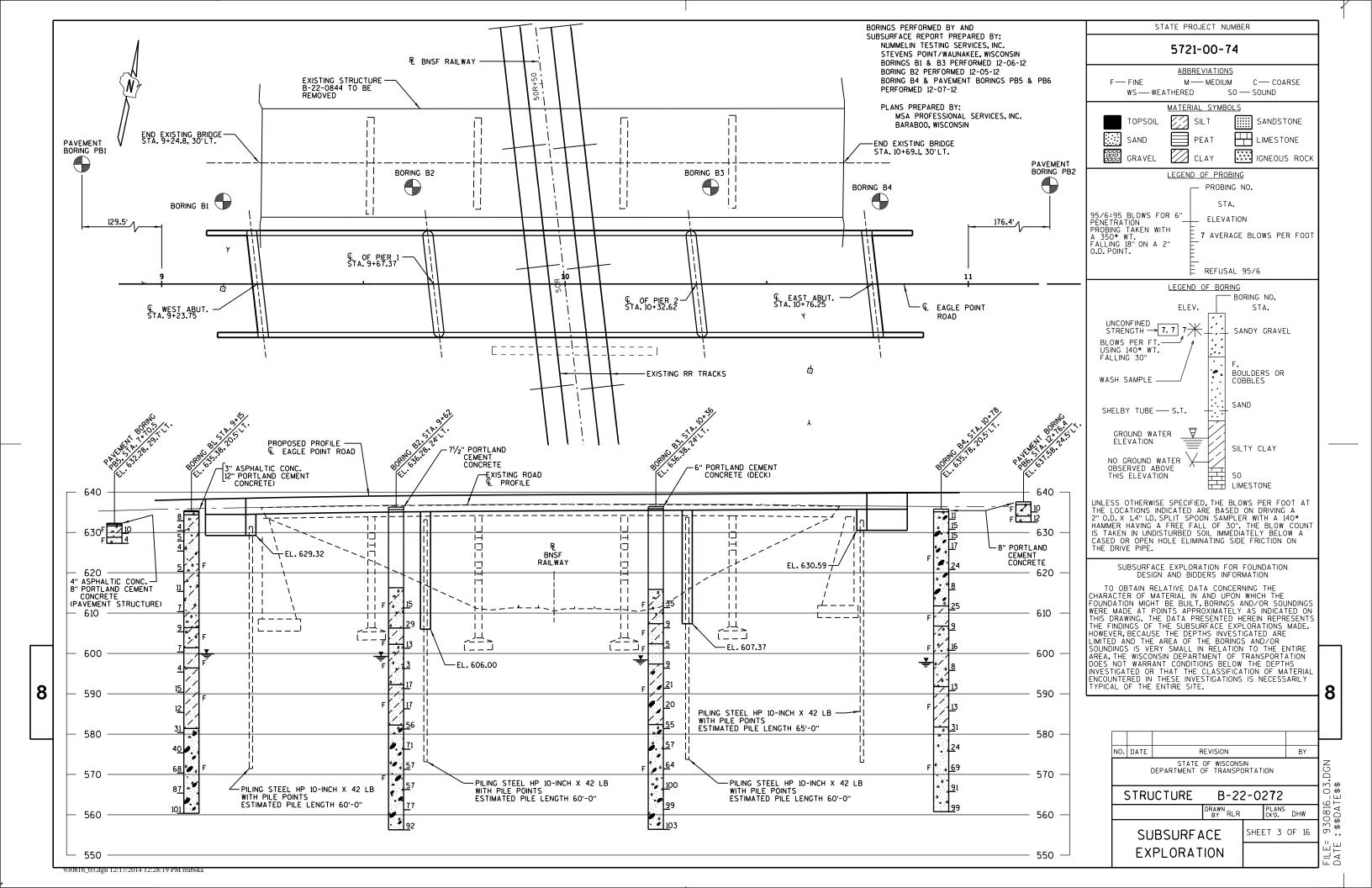


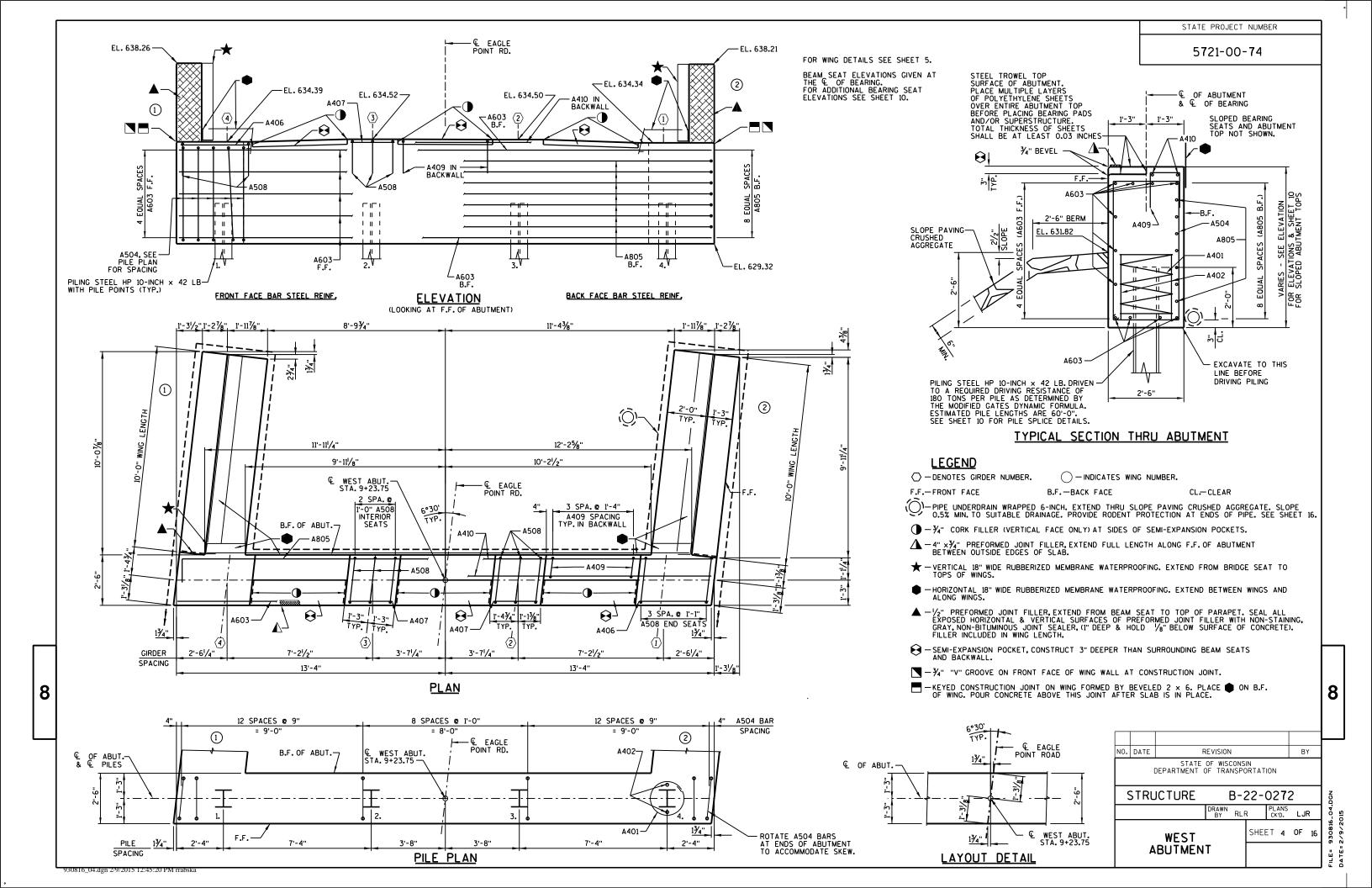
CROSS SECTION THRU BRIDGE (LOOKING EAST)

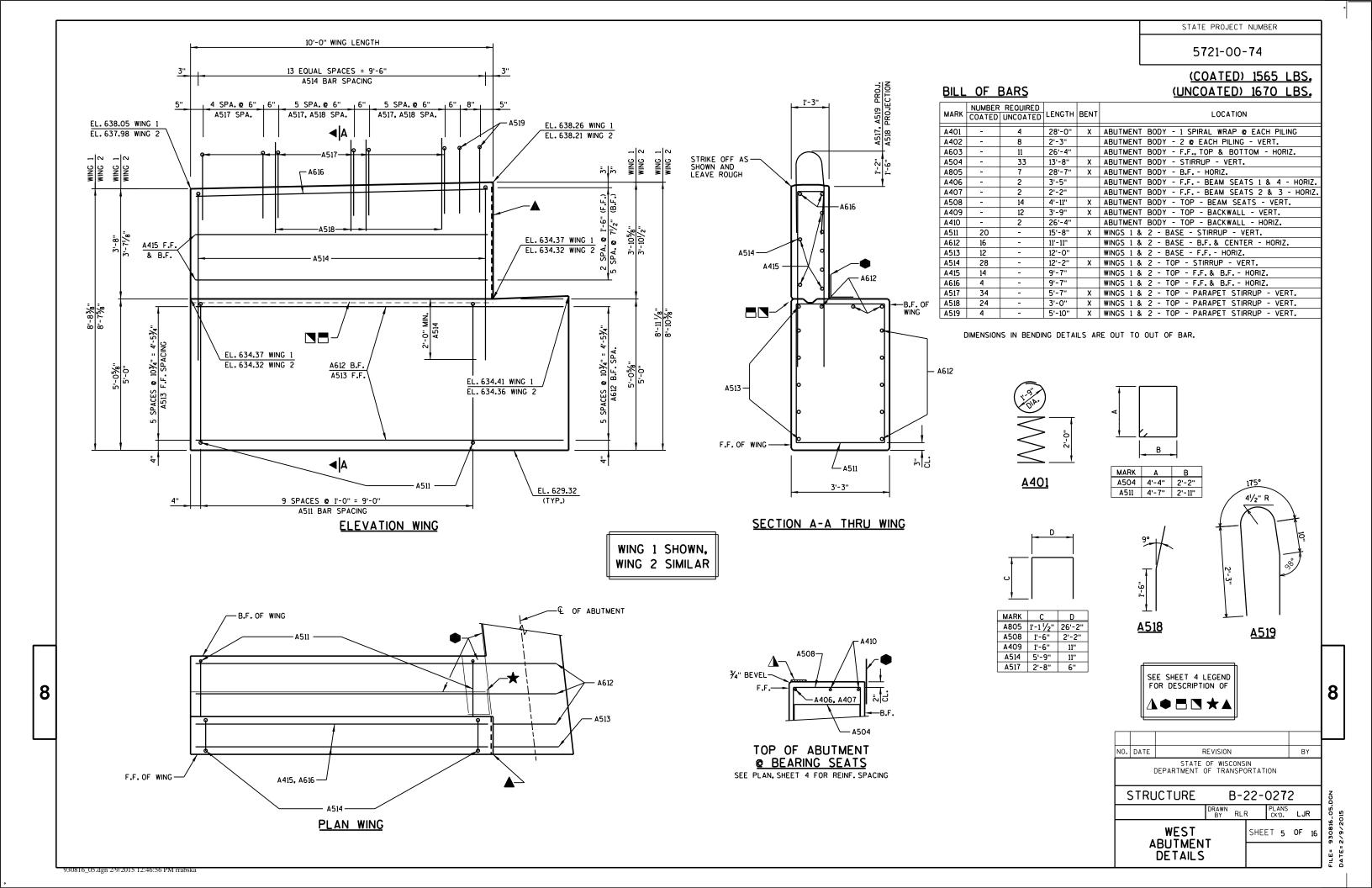
TOTAL ESTIMATED QUANTITIES

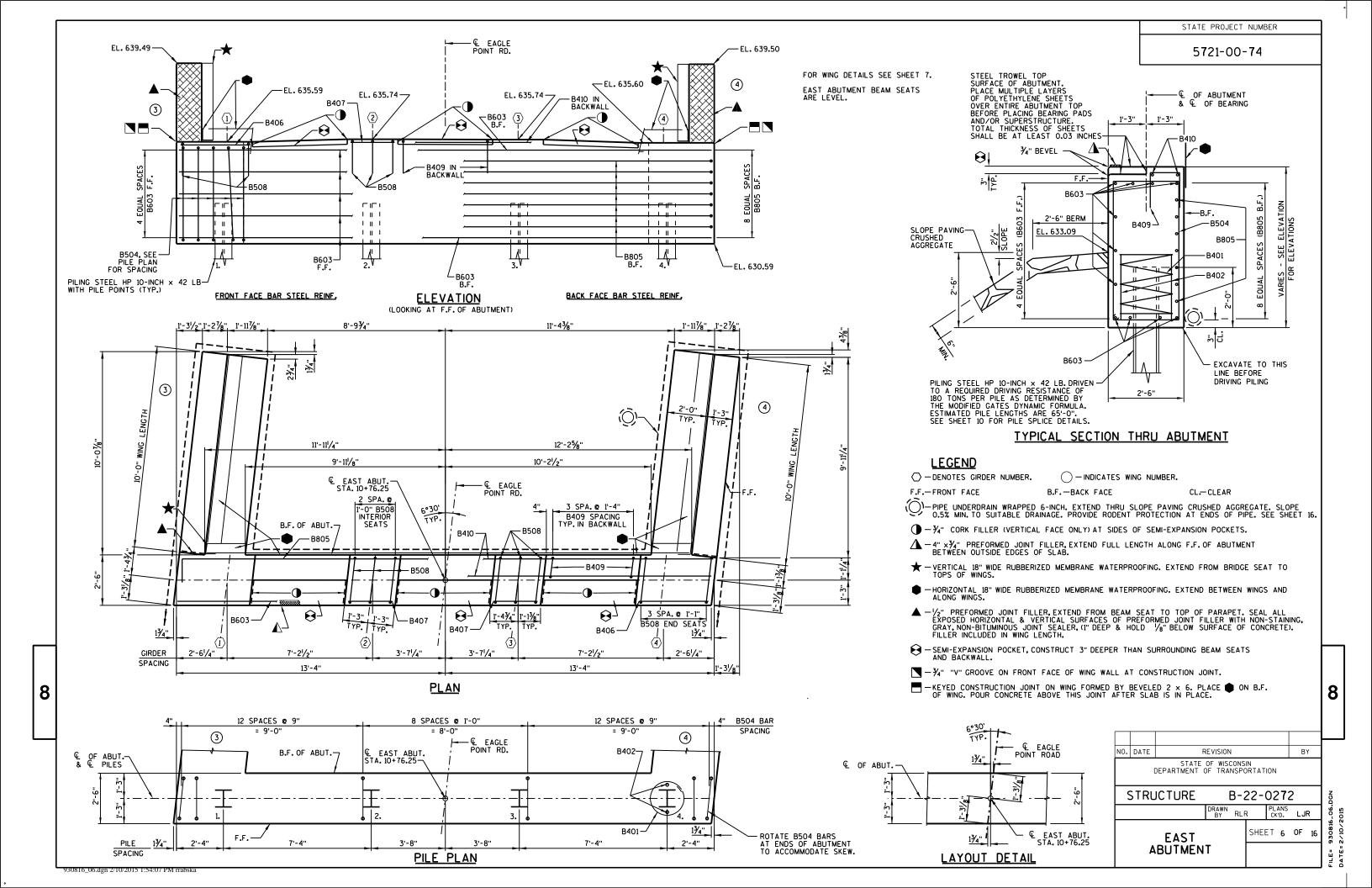
ITEM NUMBER	BID ITEM	UNIT	WEST ABUT.	PIER 1	PIER 2	EAST ABUT.	SUPER.	TOTAL
203.0200.01	REMOVING OLD STRUCTURE STATION 9+97, 30' LT	LS	-	-	-	-	-	1
203.0225.S.01	DEBRIS CONTAINMENT STRUCTURE B-22-0272	LS	-	-	-	-	-	1
206.1000.01	EXCAVATION FOR STRUCTURES BRIDGES B-22-0272	LS	-	-	-	-	-	1
210.0100	BACKFILL STRUCTURE	CY	85	-	-	85	-	170
502.0100	CONCRETE MASONRY BRIDGES	CY	28	70	68	28	176	370
502.3200	PROTECTIVE SURFACE TREATMENT	SY	-	-	-	-	560	560
503.0136	PRESTRESSED GIRDER TYPE I 36-INCH	LF	-	-	-	-	612	612
505.0405	BAR STEEL REINFORCEMENT HS BRIDGES	LB	1670	3360	3250	1670	-	9950
505.0605	BAR STEEL REINFORCEMENT HS COATED BRIDGES	LB	1565	30	30	1565	37520	40710
506.2605	BEARING PADS ELASTOMERIC NON-LAMINATED	EACH	-	-	-	-	24	24
506.4000.01	STEEL DIAPHRAGMS B-22-0272	EACH	-	-	-	-	9	9
511.1200.01	TEMPORARY SHORING B-22-0272	SF	215	465	390	185	-	1255
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	9	-	-	9	-	18
550.0500	PILE POINTS	EACH	4	7	7	4	-	22
550.1100	PILING STEEL HP 10-INCH x 42 LB	LF	240	420	420	260	-	1340
604.0500	SLOPE PAVING CRUSHED AGGREGATE	SY	155	-	-	155	-	310
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	90	-	-	90	-	180
614.0150	ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD	EACH	2	-	-	2	-	4
	NON-BID ITEMS							
	PREFORMED FILLER	SIZE	-	-	-	-	-	1/2", 3/4"
	CORK FILLER	SIZE	-			-	-	1/2"

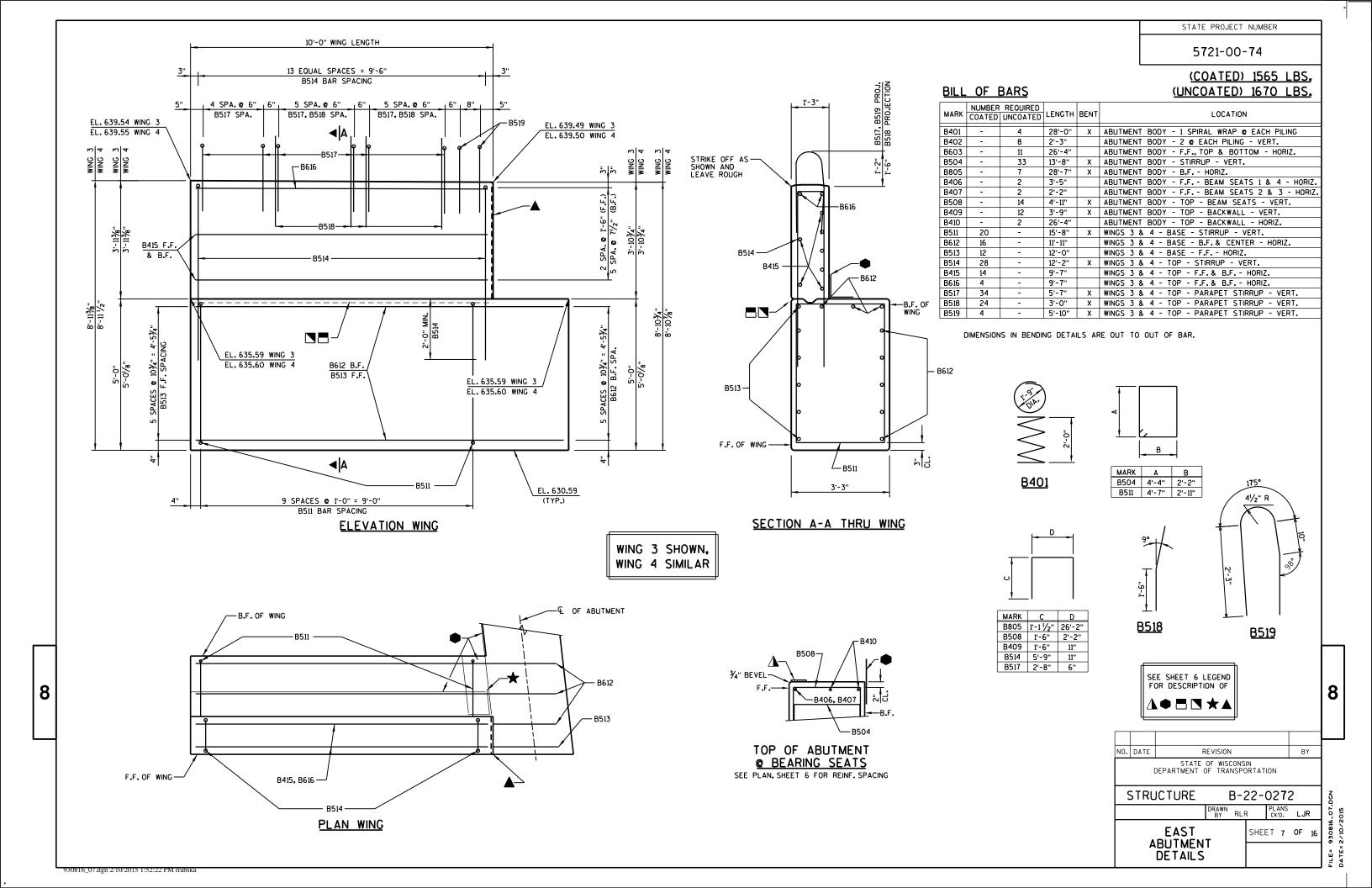
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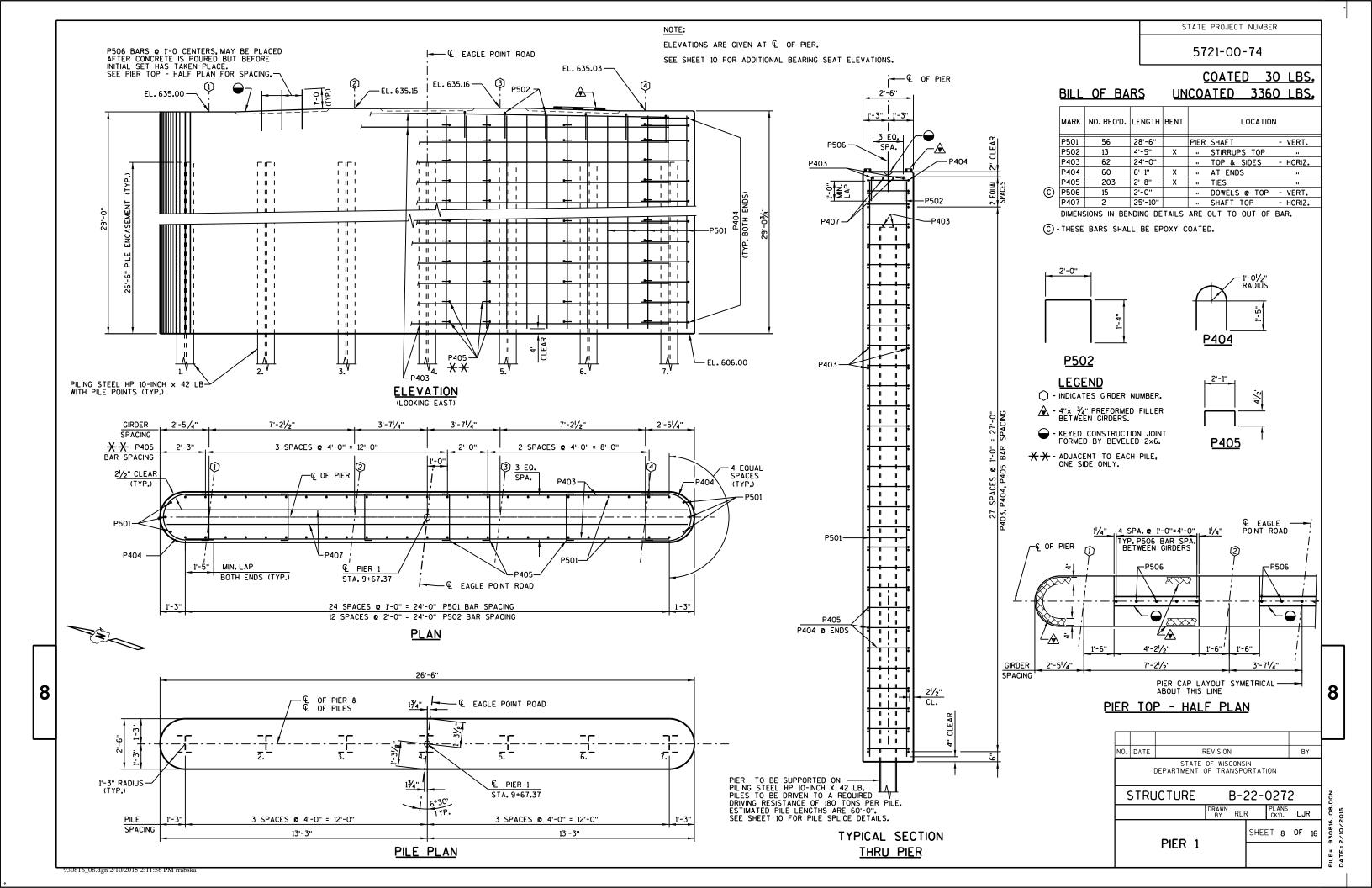


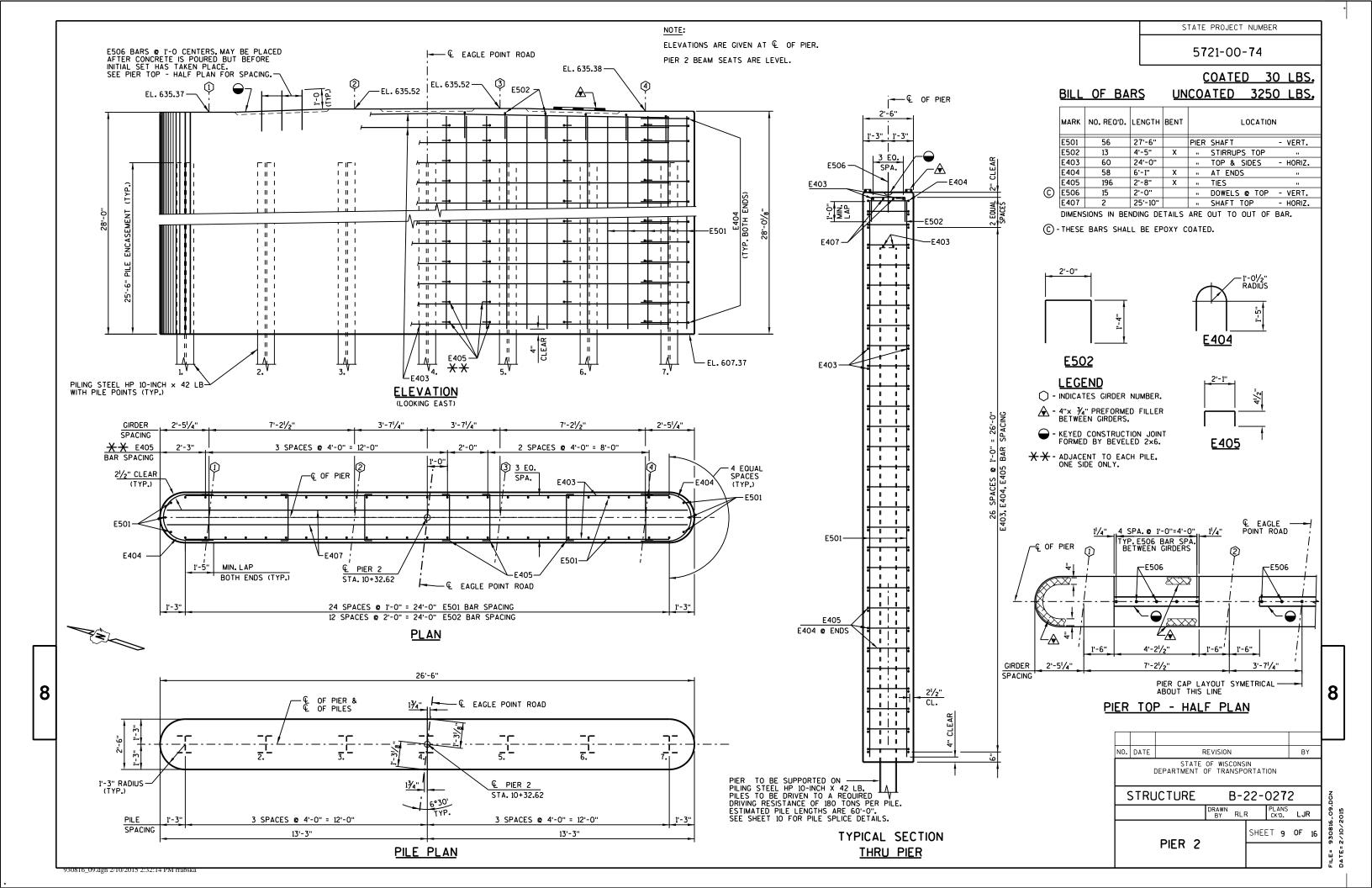


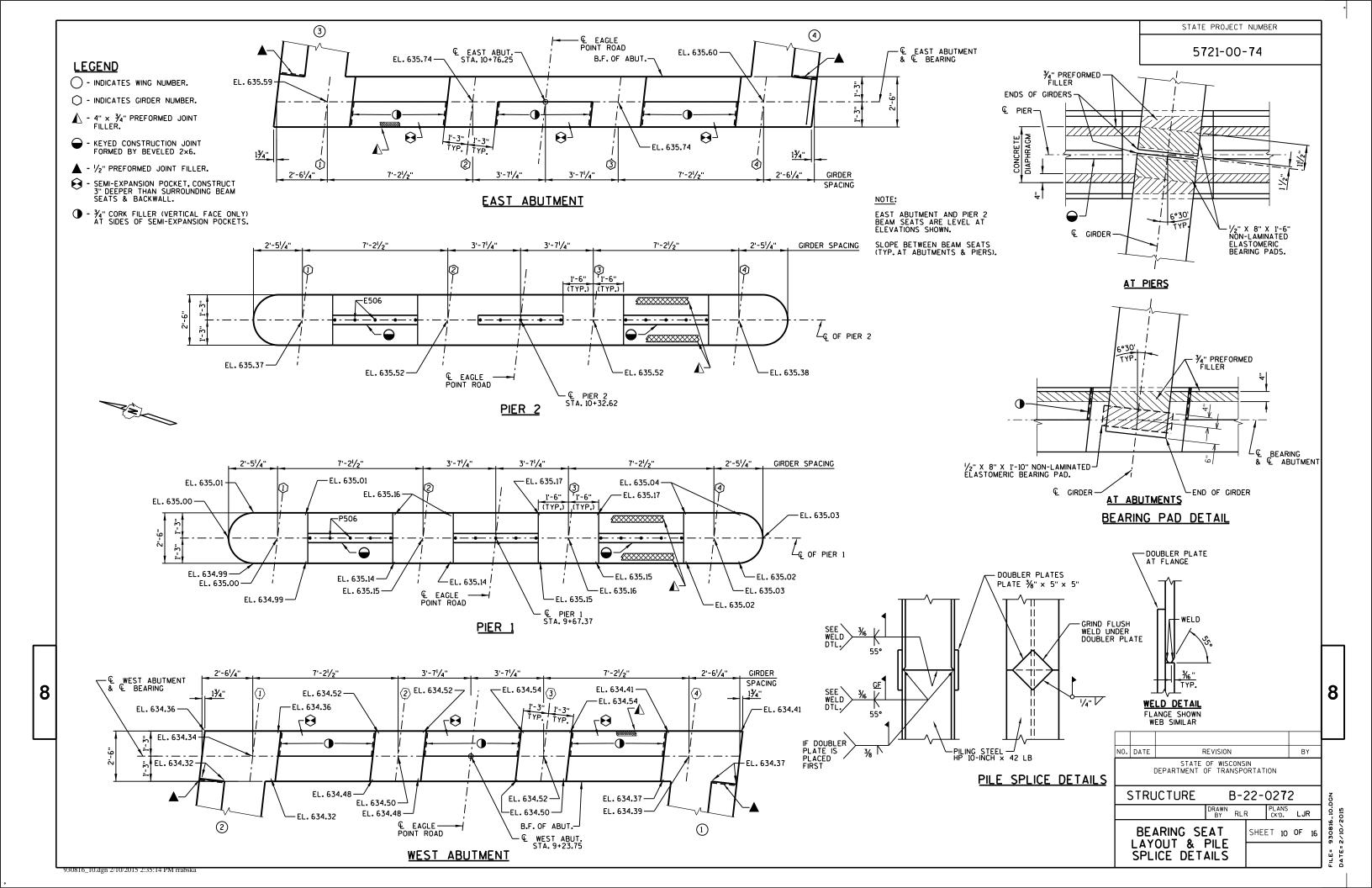


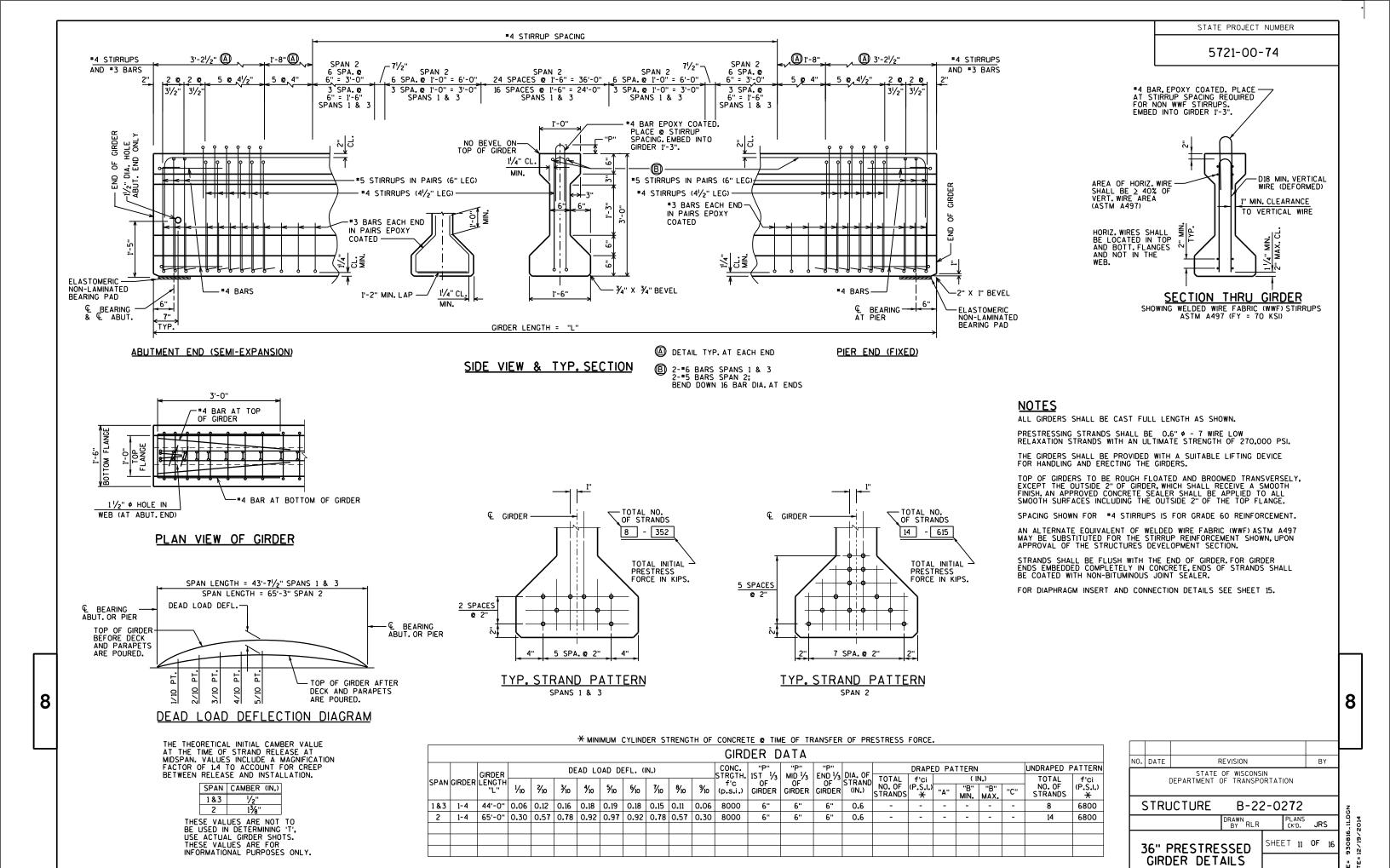


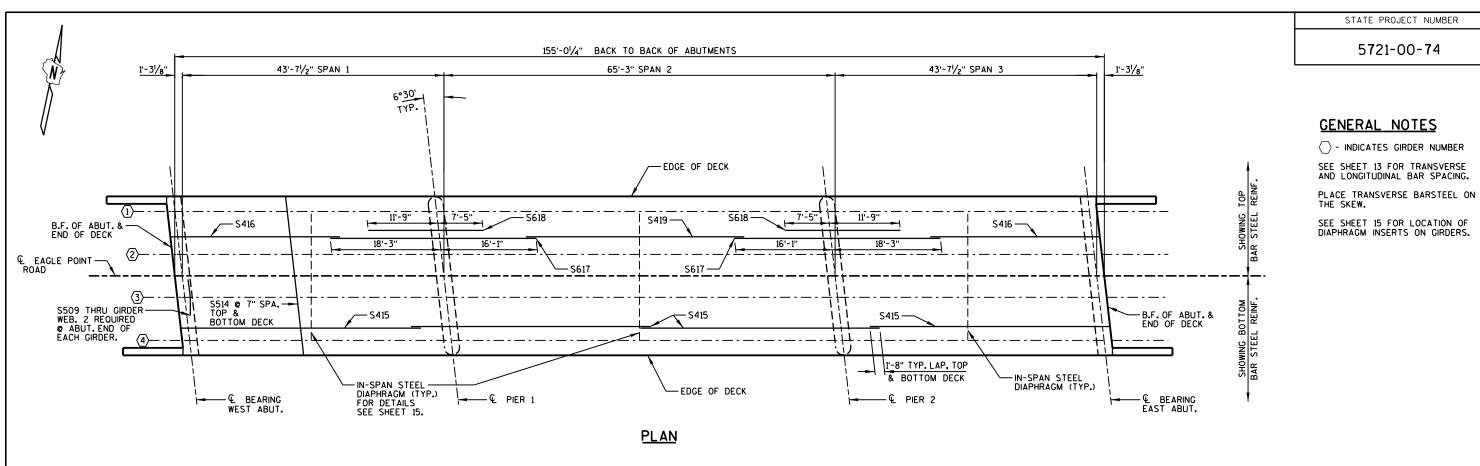






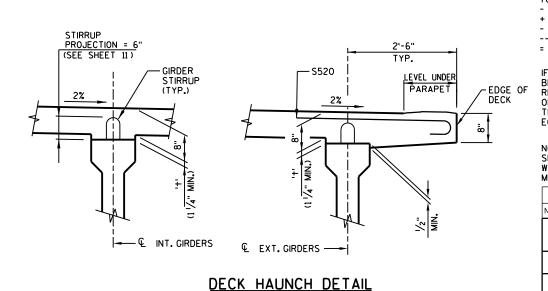






TOP OF DECK ELEVATIONS @ 4 OF GIRDERS

	SPAN	SOUTH PARAPET	C/L GIRDER	C/L GIRDER	EAGLE POINT ROAD	C/L GIRDER	C/L GIRDER	NORTH PARAPET
LOCATION	POINT	FACE	4	3	C/L	2	1	FACE
W. ABUT.	1	638.29	638.31	638.44	638.50	638.42	638.26	638.23
	1.1	638.37	638.39	638.52	638.59	638.51	638.35	638.32
	1.2	638.45	638.47	638.60	638.67	638.59	638.43	638.40
	1.3	638.53	638.55	638.68	638.74	638.67	638.51	638.48
	1.4	638.60	638.62	638.75	638.82	638.74	638.58	638.55
	1.5	638.66	638.69	638.82	638.88	638.80	638.65	638.62
	1.6	638.72	638.75	638.88	638.94	638.87	638.71	638.69
	1.7	638.78	638.80	638.94	639.00	638.93	638.77	638.74
	1.8	638.83	638.85	638.99	639.05	638.98	638.83	638.80
	1.9	638.88	638.90	639.04	639.10	639.03	638.87	638.85
PIER 1	2	638.92	638.94	639.08	639.15	639.07	638.92	638.89
	2.1	638.97	639.00	639.13	639.20	639.13	638.98	638.95
	2.2	639.01	639.04	639.18	639.25	639.17	639.02	639.00
	2.3	639.05	639.07	639.21	639.28	639.21	639.06	639.03
	2.4	639.08	639.10	639.24	639.31	639.24	639.09	639.07
	2.5	639.11	639.14	639.28	639.35	639.27	639.13	639.10
	2.6	639.15	639.17	639.31	639.38	639.31	639.16	639.13
	2.7	639.18	639.20	639.34	639.41	639.34	639.19	639.16
	2.8	639.21	639.24	639.37	639.44	639.37	639.22	639.20
	2.9	639.24	639.27	639.41	639.48	639.40	639.26	639.23
PIER 2	3	639.28	639.30	639.44	639.51	639.44	639.29	639.26
	3.1	639.30	639.32	639.46	639.53	639.46	639.31	639.28
	3.2	639.32	639.34	639.48	639.55	639.48	639.33	639.31
	3.3	639.34	639.37	639.51	639.57	639.50	639.35	639.33
	3.4	639.36	639.39	639.53	639.60	639.52	639.38	639.35
	3.5	639.39	639.41	639.55	639.62	639.54	639.40	639.37
	3.6	639.41	639.43	639.57	639.64	639.57	639.42	639.39
	3.7	639.43	639.45	639.59	639.66	639.59	639.44	639.42
	3.8	639.45	639.48	639.61	639.68	639.61	639.46	639.44
	3.9	639.47	639.50	639.64	639.71	639.63	639.48	639.46
E. ABUT	4	639.49	639.52	639.66	639.73	639.65	639.51	639.48



TO DETERMINE '+', ELEV. OF TOP OF GIRDERS AT & OF SUBSTRUCTURE UNITS & AT 1/10 POINTS OF EACH SPAN SHALL BE TAKEN. TO DETERMINE THE TOP OF DECK ELEVATION FOR POINT REFERRED USE TABLE ON THIS SHEET AND ADJUST FOR CROSS SLOPE OVER GIRDER. THEN FOLLOW THIS PROCESS:

TOP OF DECK ELEV. AT FINAL GRADE
- TOP OF GIRDER ELEVATION
+ DEADLOAD DEFLECTION (SEE SHEET 11)

- DECK THICKNESS

= HAUNCH HEIGHT 't'

IF 1 1/4" MINIMUM HAUNCH HEIGHT '+' CANNOT BE MAINTAINED, THE GRADE LINE MAY BE REVISED BY THE ENGINEER AT THE OPTION OF THE CONTRACTOR. THE PLAN DECK
THICKNESS SHALL BE HELD. MAX. HAUNCH HEIGHT
EQUALS "STIRRUP PROJECTION" MINUS 3".

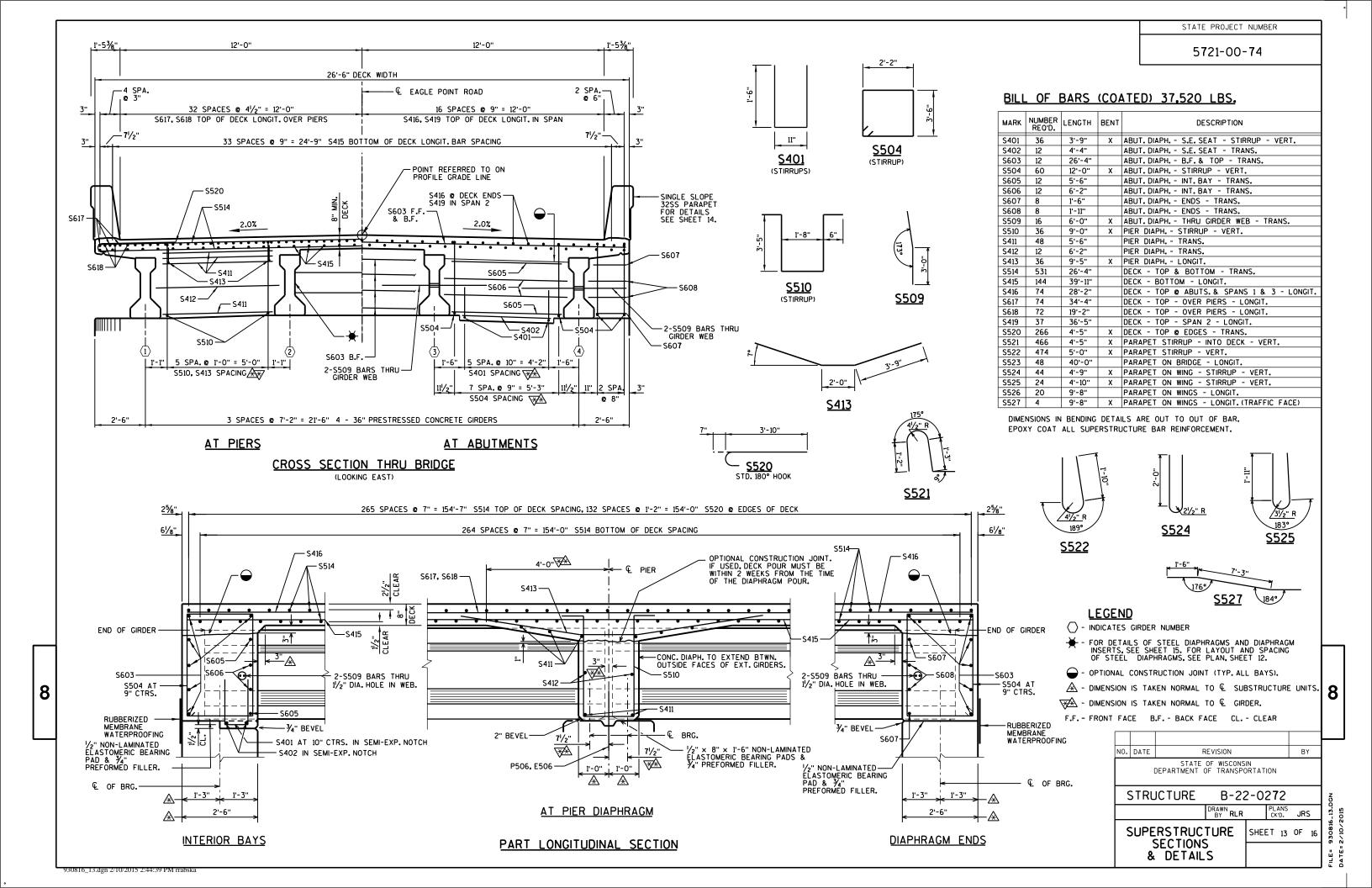
NOTE: AN AVERAGE HAUNCH ("+") OF 2¾" FOR SPAN 1 AND 2¼" FOR SPANS 2 & 3 WAS USED IN THE QUANTITY "CONCRETE MASONRY STRUCTURES."

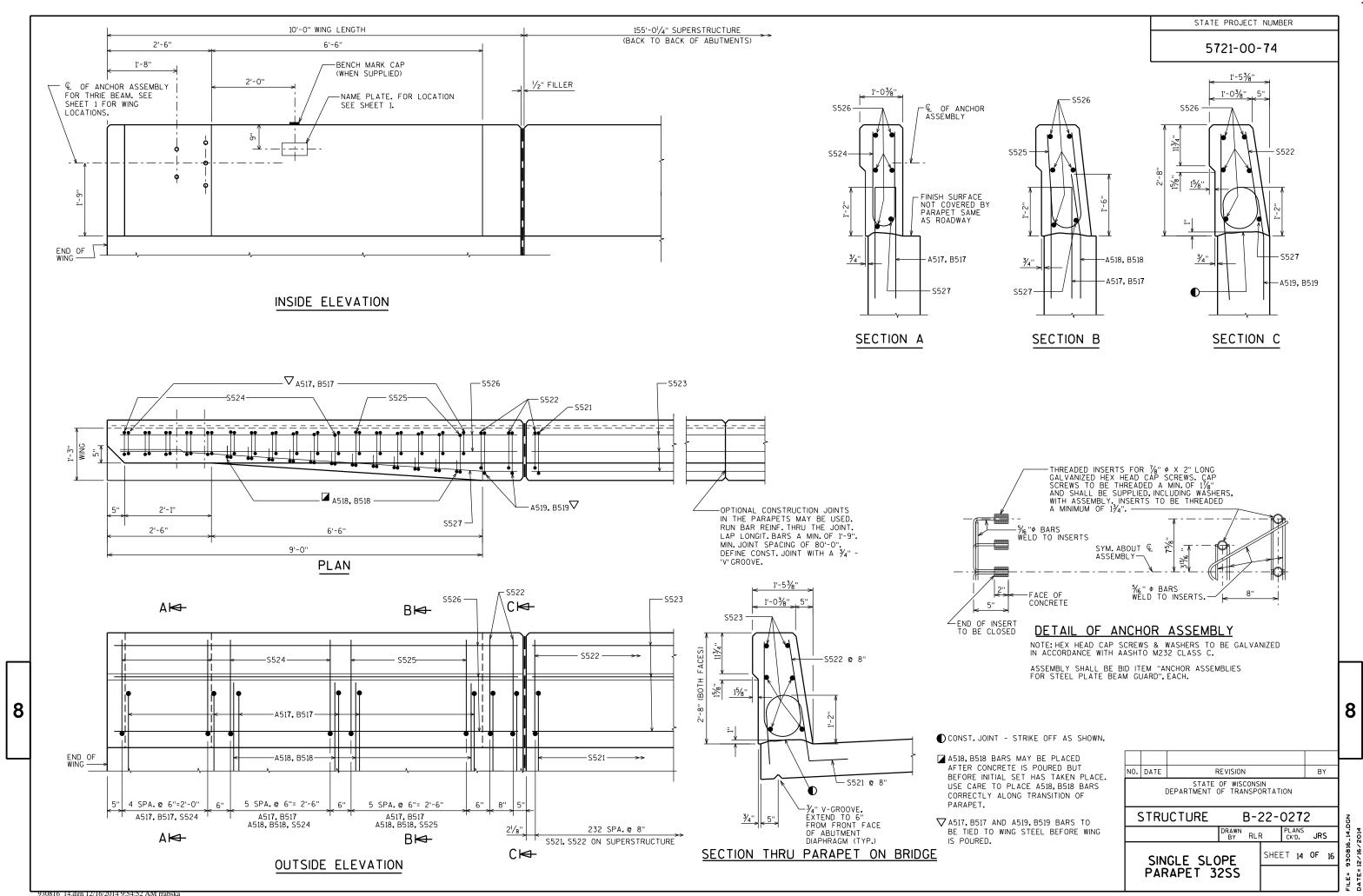
BY STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION STRUCTURE B-22-0272 DRAWN BY RLR JRS

SUPERSTRUCTURE

SHEET 12 OF 16

8





5721-00-74

NOTES

ALL DIAPHRAGM MATERIAL NOT EMBEDDED IN THE CONCRETE GIRDER SHALL BE PAID FOR AT THE UNIT PRICE BID FOR "STEEL DIAPHRAGMS B-22-0272", EACH.

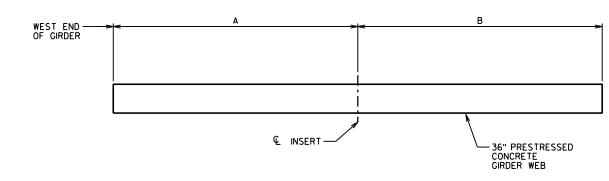
EACH DIAPHRAGM BETWEEN GIRDERS SHALL CONSTITUTE ONE UNIT.

ALL DIAPHRAGM STRUCTURAL STEEL SHALL BE ASTM A709 GRADE 36.

ALL BOLTS, NUTS AND WASHERS SHALL BE ASTM A325 TYPE 1.

ALL DIAPHRAGM STRUCTURAL STEEL SHOWN SHALL BE HOT-DIPPED GALVANIZED. ALL BOLTS, NUTS AND WASHERS SHALL BE HOT-DIPPED GALVANIZED IN ACCORDANCE WITH ASTM A153 CLASS C. GALVANIZED NUTS SHALL BE TAPPED OVERSIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM A563 AND SHALL MEET THE REQUIREMENTS OF SUPPLEMENTARY REQUIREMENT S1 OF ASTM A563, LUBRICANT AND TEST FOR COATED NUTS

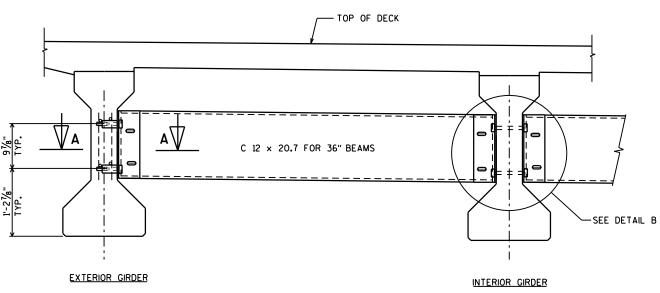
SEE SHEET 12 FOR LOCATION OF DIAPHRAGMS.

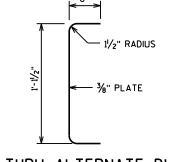


DIAPHRAGM INSERT LOCATION PLAN

DIAPHRAGM INSERT LOCATION TABLE

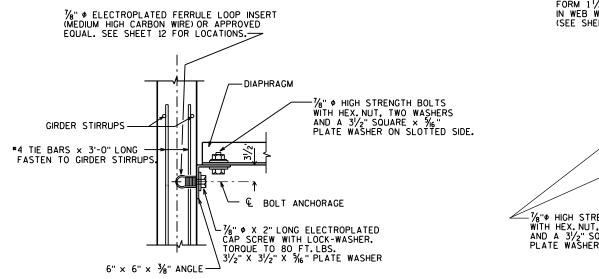
SPAN	GIRDER NUMBER	Α	В	INSERT TYPE
	1	23'-2¾"	20'-91/4"	FERRULE LOOP
,	2	22'-4%"	21'-7 ¹ /8"	PIPE SLEEVE
	3	21'-71/8"	22'-4%"	PIPE SLEEVE
	4	20'-91/4"	23'-2¾"	FERRULE LOOP
	1	33'-8¾"	31'-3'/4"	FERRULE LOOP
2	2	32'-10%"	32'-1 1/8"	PIPE SLEEVE
	3	32'-1 1/8"	32'-10%"	PIPE SLEEVE
	4	31'-3'/4"	33'-8¾"	FERRULE LOOP
	1	23'-2¾"	20'-91/4"	FERRULE LOOP
3	2	22'-4%"	21'-7 ¹ /8"	PIPE SLEEVE
٦	3	21'-71/8"	22'-4%"	PIPE SLEEVE
	4	20'-91/4"	23'-2¾"	FERRULE LOOP

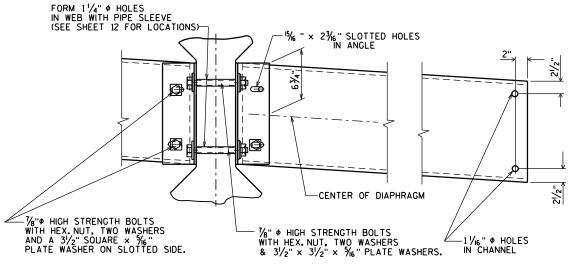




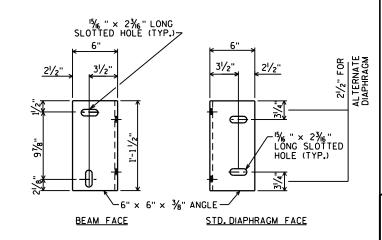
PART TRANSVERSE SECTION AT DIAPHRAGM

SECTION THRU ALTERNATE DIAPHRAGM





DETAIL B (FOR CONTINUOUS LINE OF DIAPHRAGMS)

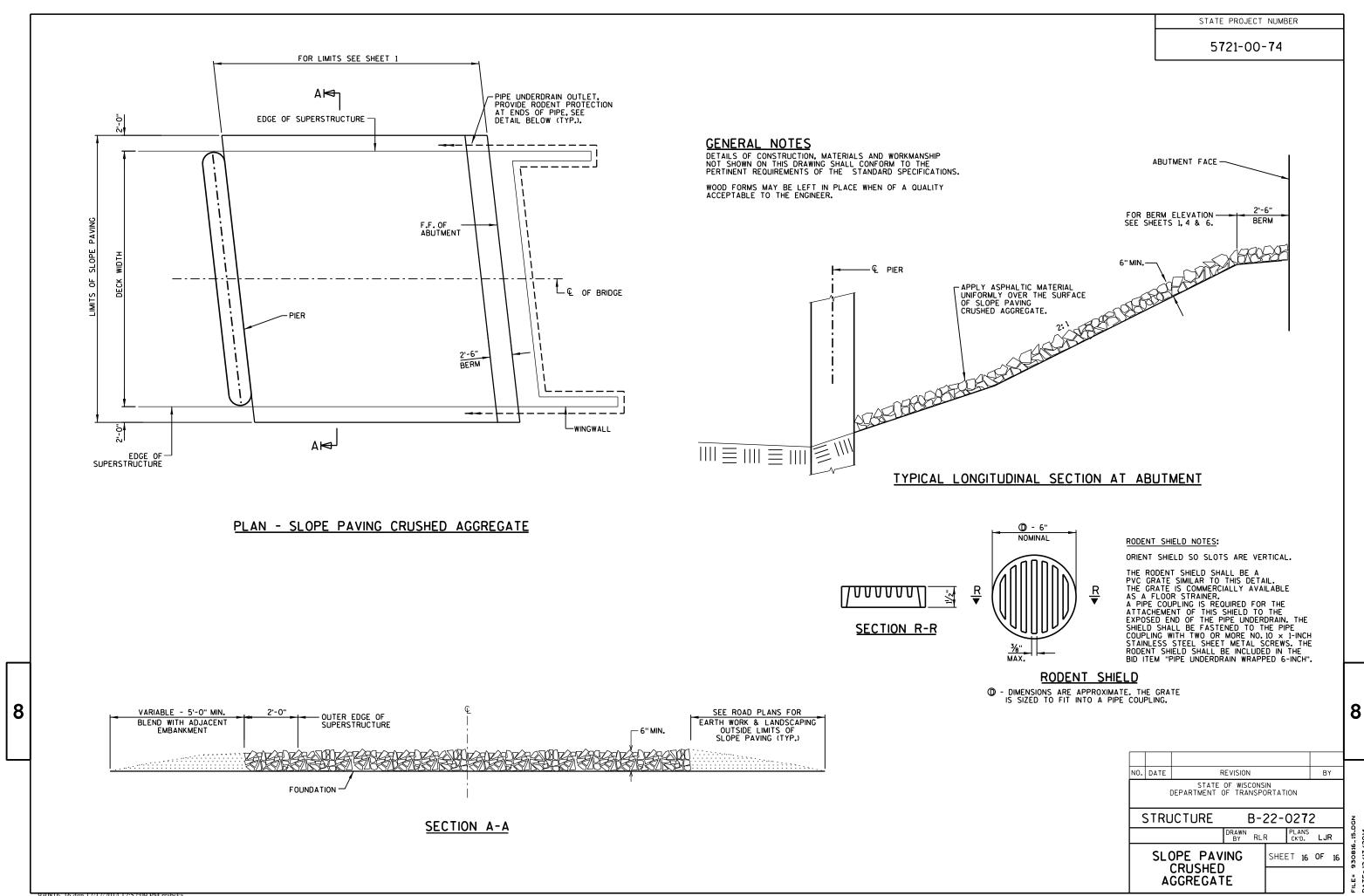


DIAPHRAGM SUPPORT

NO.	DATE	F	REVISION	1			В	Y	
	[STATE DEPARTMENT (OF WISO			ION			
	TRL	JCTURE	В.	-22	2-0	272			z
			DRAWN BY	RLF	₹	PLANS CK'D.	JR	S	15.00
		STEEL			SHE	ET 15	OF	16	930816
	D	IAPHRAG	М						ׅ֓֞֟֟֟֟֝֟֟֝֟֟֝֟֝֟֟֝֟֝֟֡֟֝֟֡֝֟֡֡֡֡֡֡֝֟֡֝ ֡

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SECT. A-A (FOR EXTERIOR ATTACHMENT)



3+50		VOLUME (C.Y.) CUT FILL	ADJ. FILL
	0	0	0
4+00 50			0
			0
		0 0	0
			0
			0
			0
	180 0		0
	164.7 0		0
7+86	107.5 0		
8+11	121.3 0		0 0
8+40 25	130.8 0		0 0
8+65	152.5 0		
9+00	174.9 0		9 0
9+13	174.9 0		
			0
			0
			0
40.07			
		0 66	0
			0
			11
			23
		4 35	45
11+89	7.2 10.4	11 23	30
12+14	9.1		12
12+50	8.8		2 5
13+00	5.5 9.2		2 66
13+50 50	6.3 9		2 60
	4.9		14
			0
			0
	0 0		0
			0
			0
	0 0		
TOTAL - STAGE 2			214 214
	STAGE	0 12.9 50 22.8 50 82.8 50 165.5 50 166.5 50 166.3 50 166.3	0

FILE NAME : PLOT DATE : PLOT BY : PLOT NAME : ORG DATE : ORIGINATOR : PLOT SCALE : 1:1

EARTHWORK

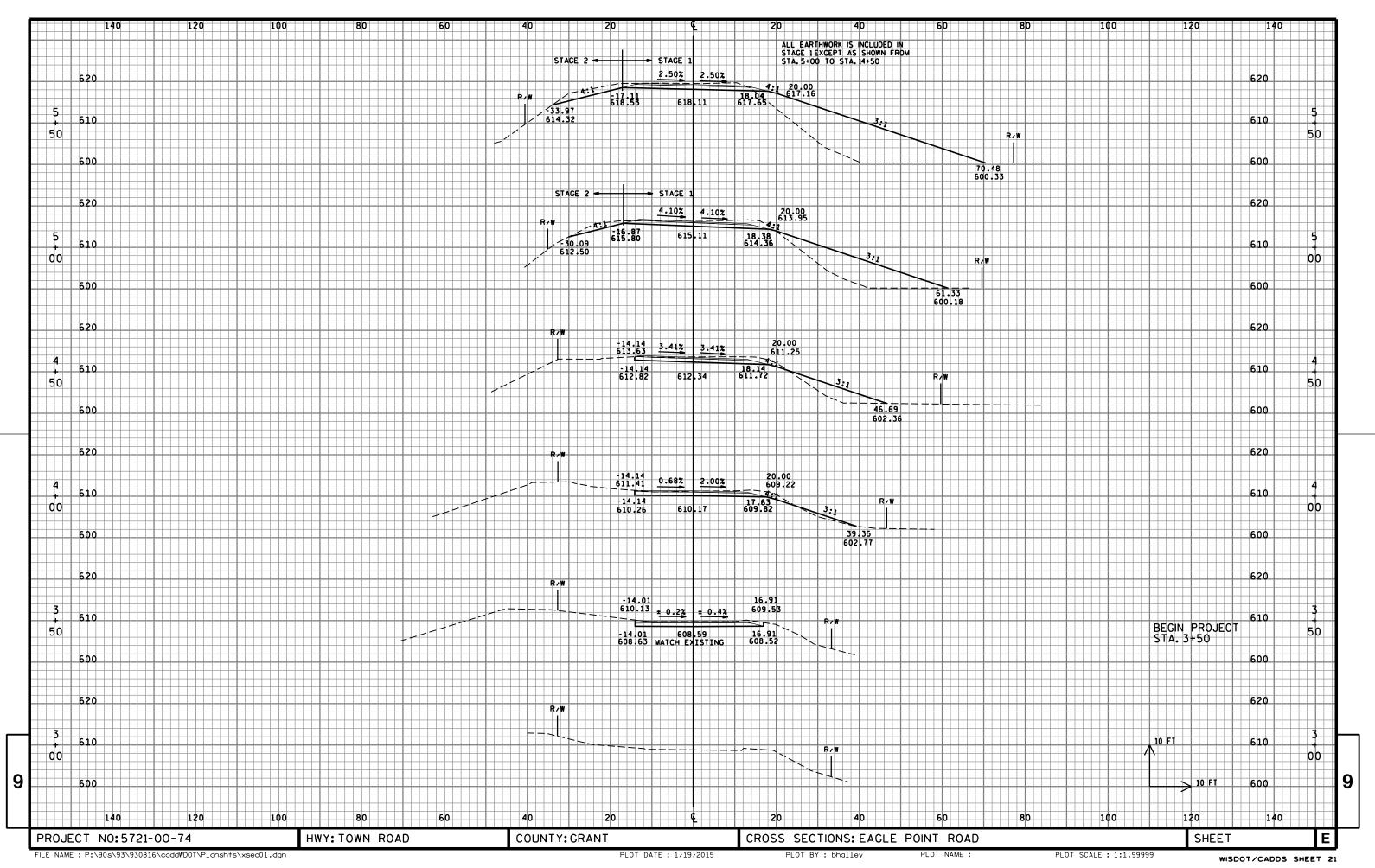
COUNTY: GRANT

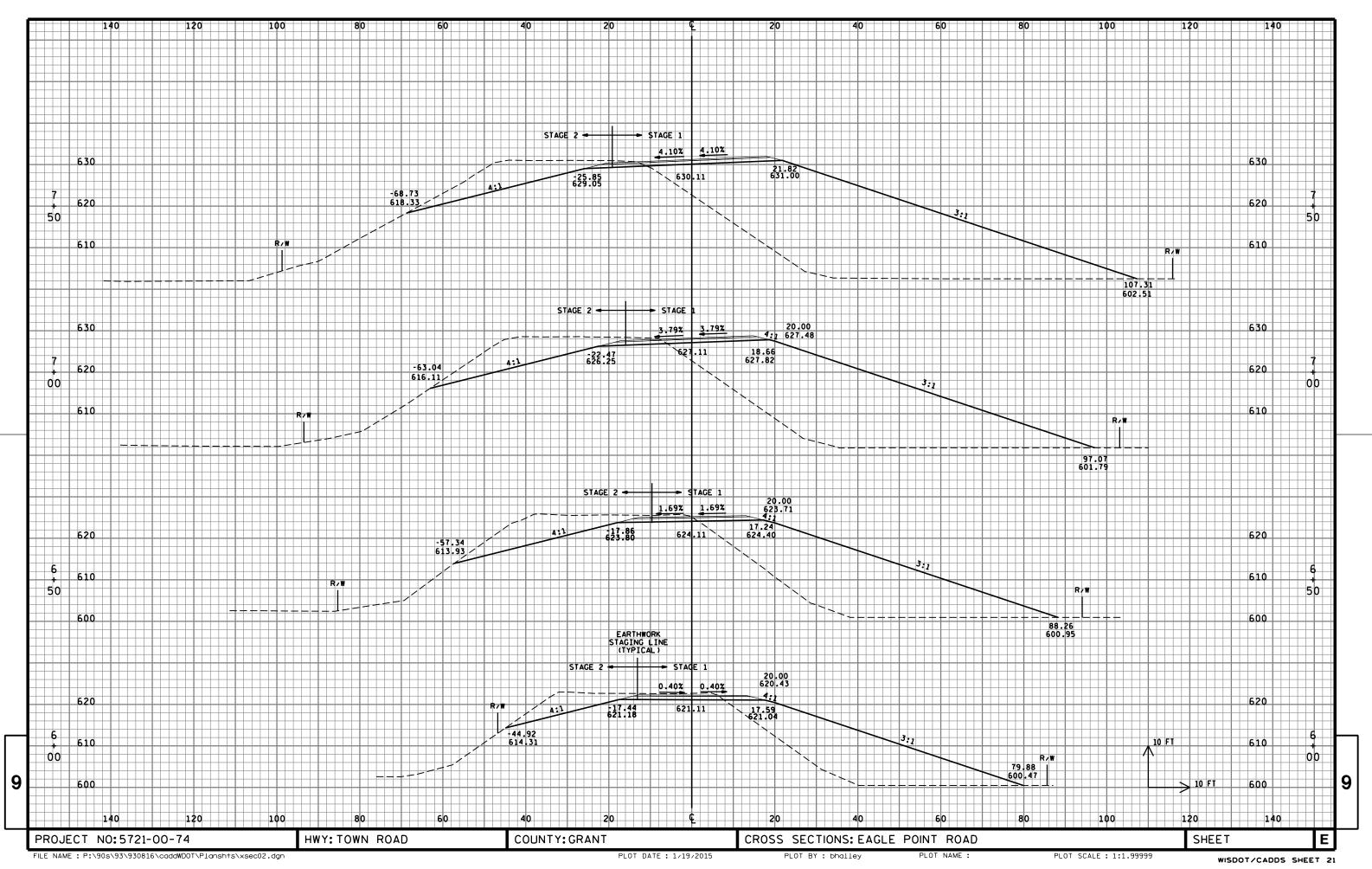
HWY: TOWN ROAD

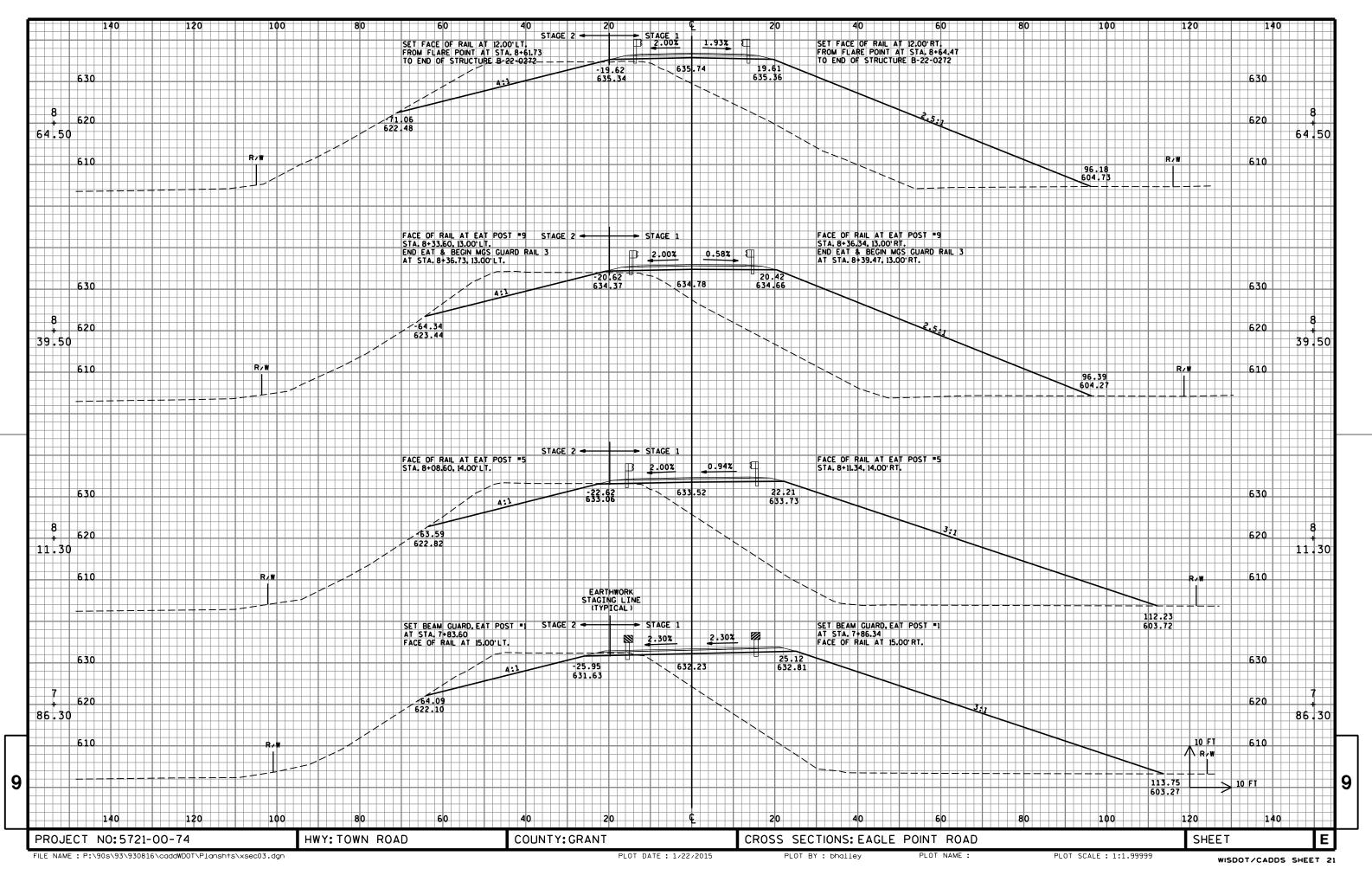
PROJECT NO: 5721-00-74

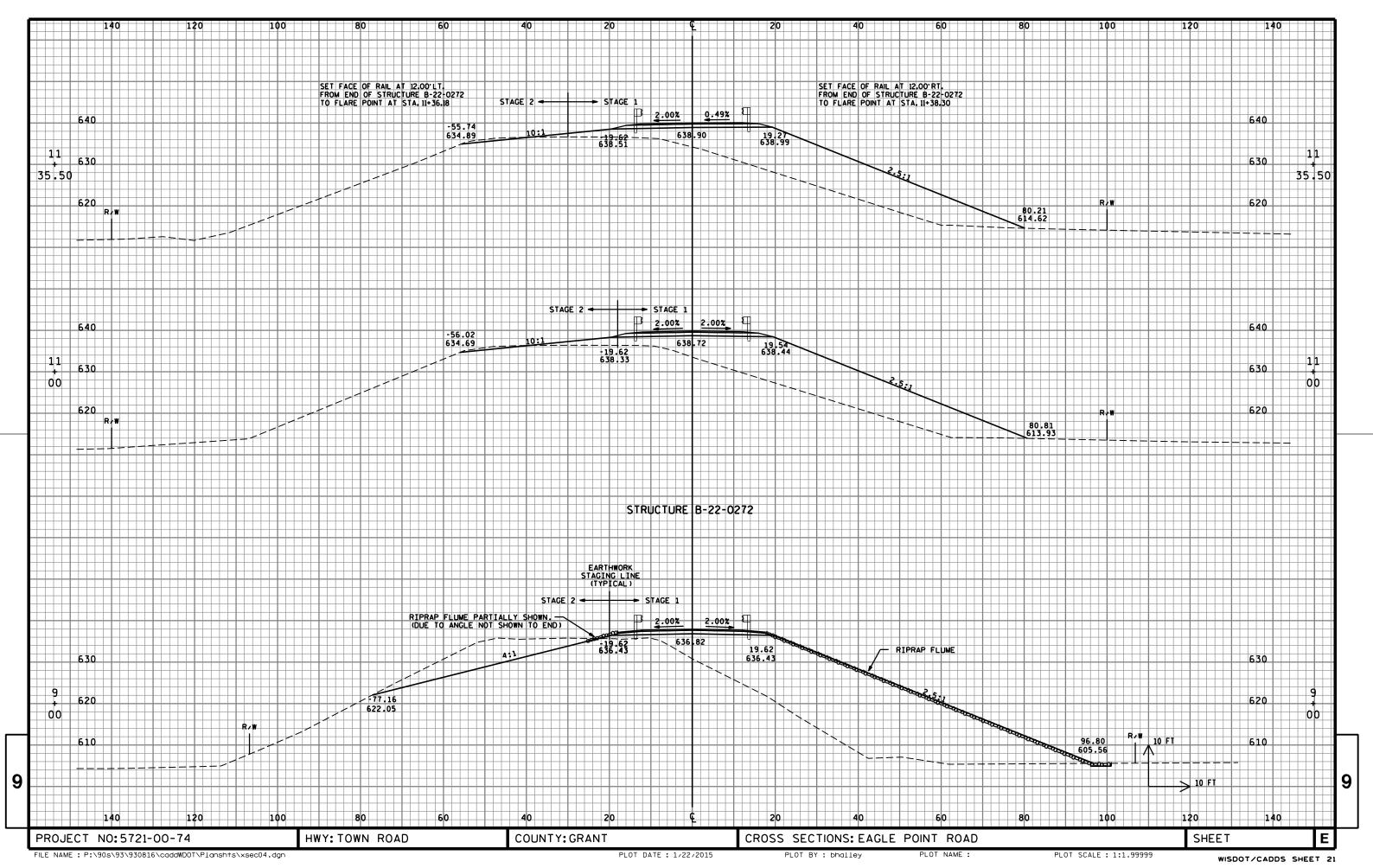
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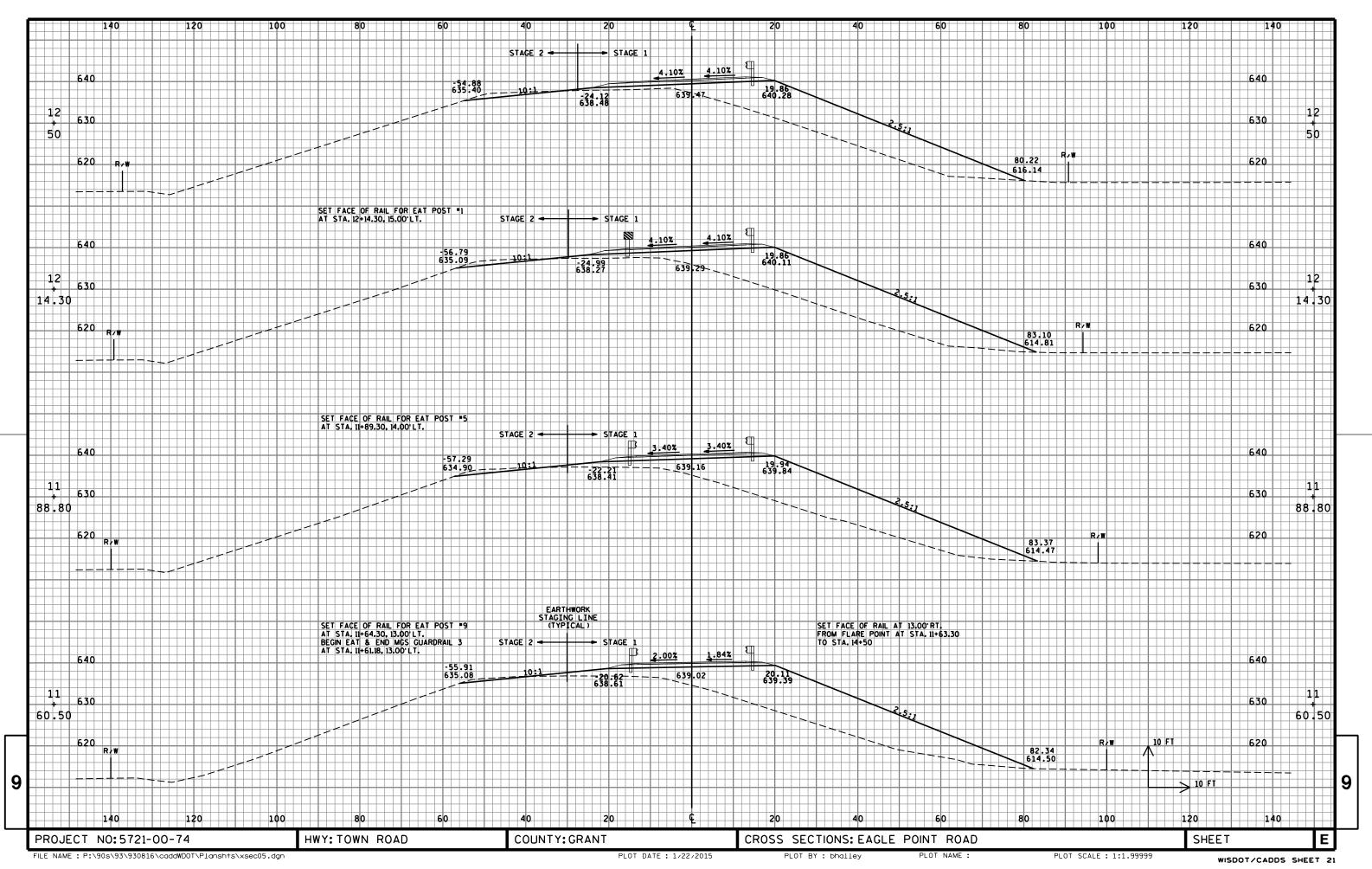
SHEET NO:

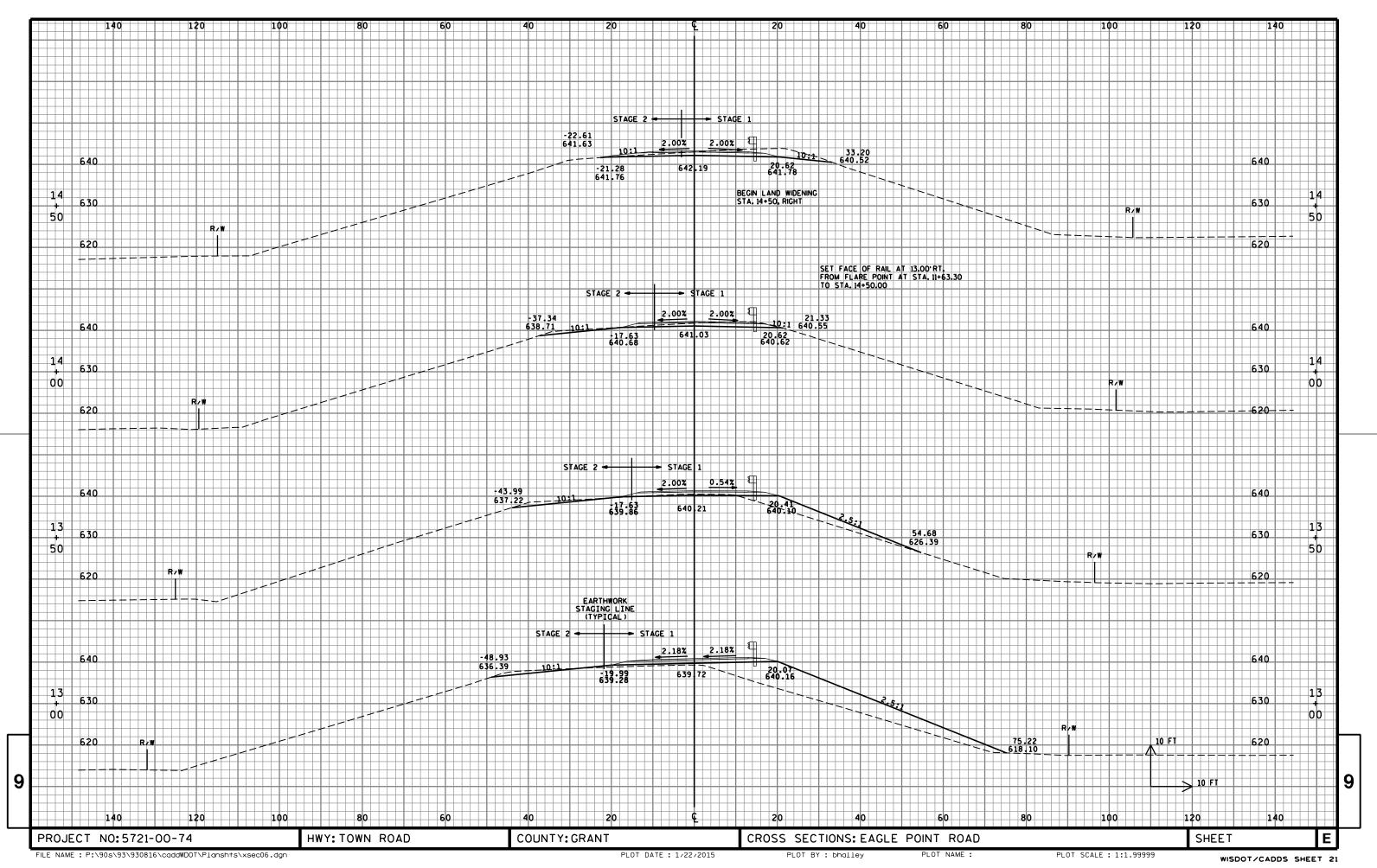


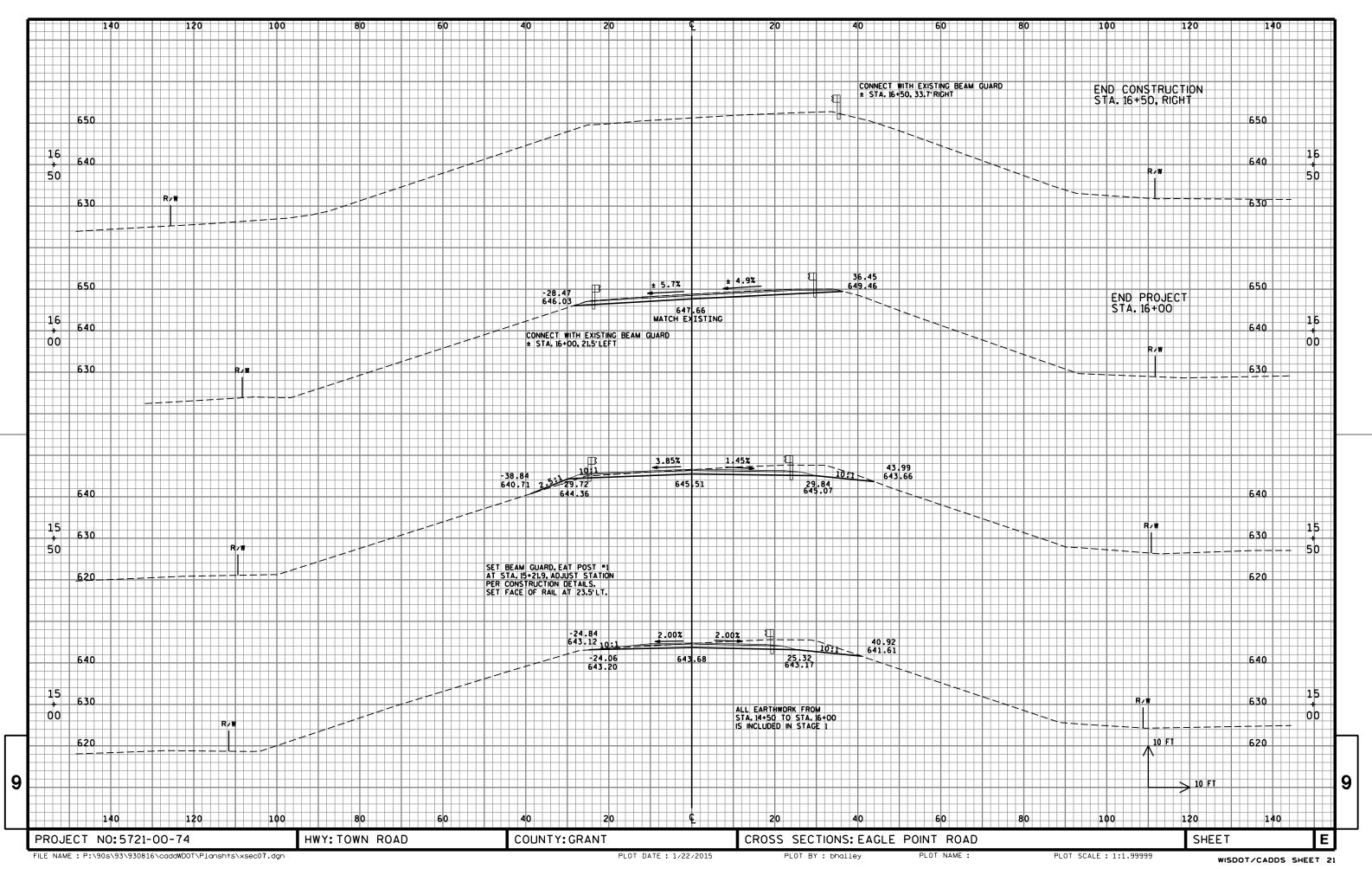












Notes



Wisconsin Department of Transportation

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