

GENERAL NOTES

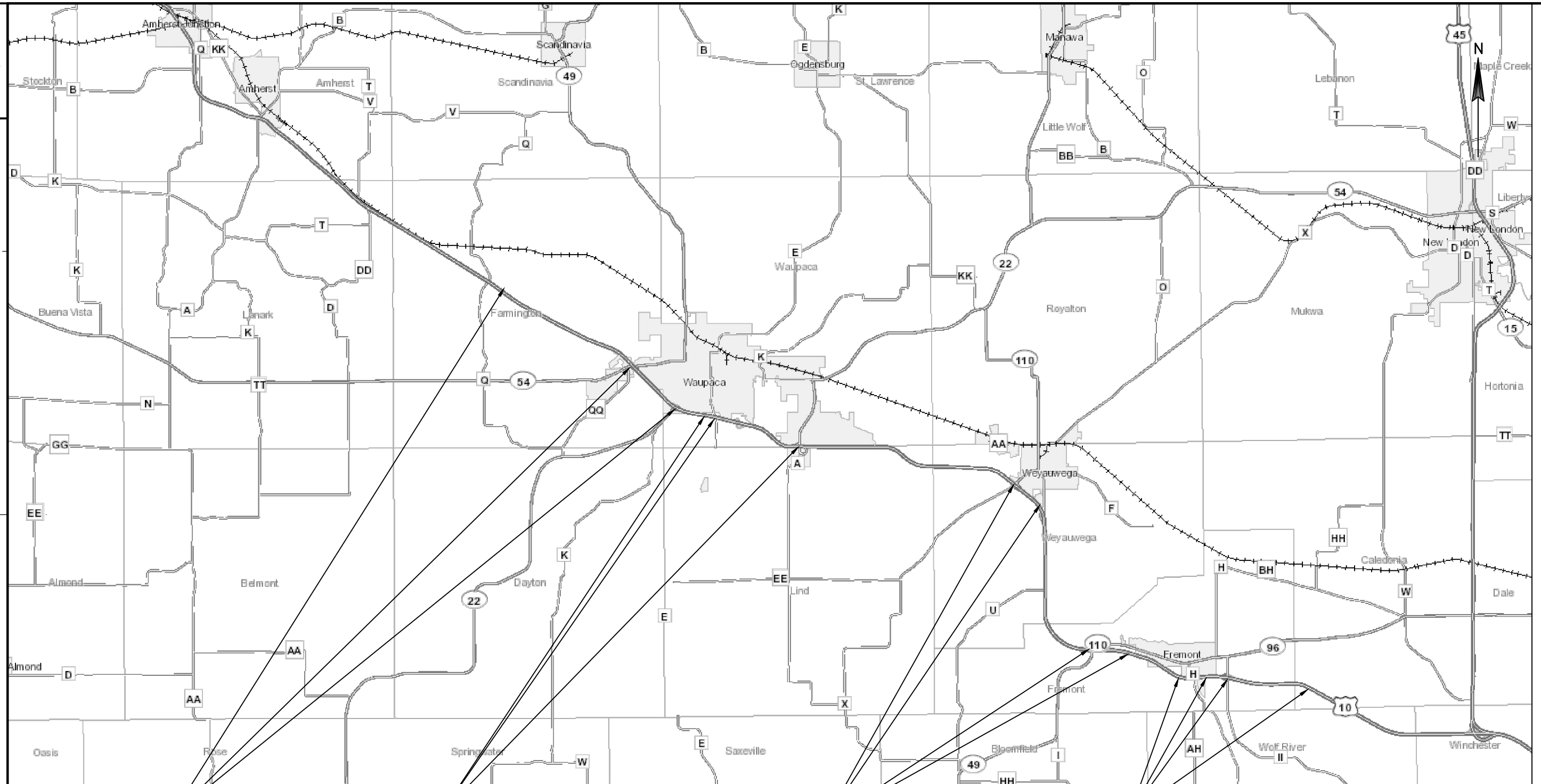
THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS.
REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

THE LOCATION OF EXISTING UTILITY FACILITIES LOCATED WITHIN THE PROJECT ARE NOT
SHOWN ON THE PLANS.

DNR LIASON

DNR CONTACT
BOBBI JO FISHER
WAUPACA, WAUSHARA, MARQUETTE, GREEN LAKE
ENVIROMENTAL ANALYST & REVIEW SPECIALIST
DEPARTMENT OF NATURAL RESOURCES
PHONE: 920-787-3015





B-68-19
B-68-73
B-68-77
B-68-74
B-68-21
B-68-75

B-68-22
B-68-76
B-68-112
B-68-121

B-68-83
B-68-84
B-68-104
B-68-105
B-68-92
B-68-93
B-68-94
B-68-95

B-68-96
B-68-97
B-68-89
B-68-90
B-68-98
B-68-99
B-68-109
B-68-110

SINGLE LANE FREEWAY CLOSURE

PROJECT NO:1009-42-62

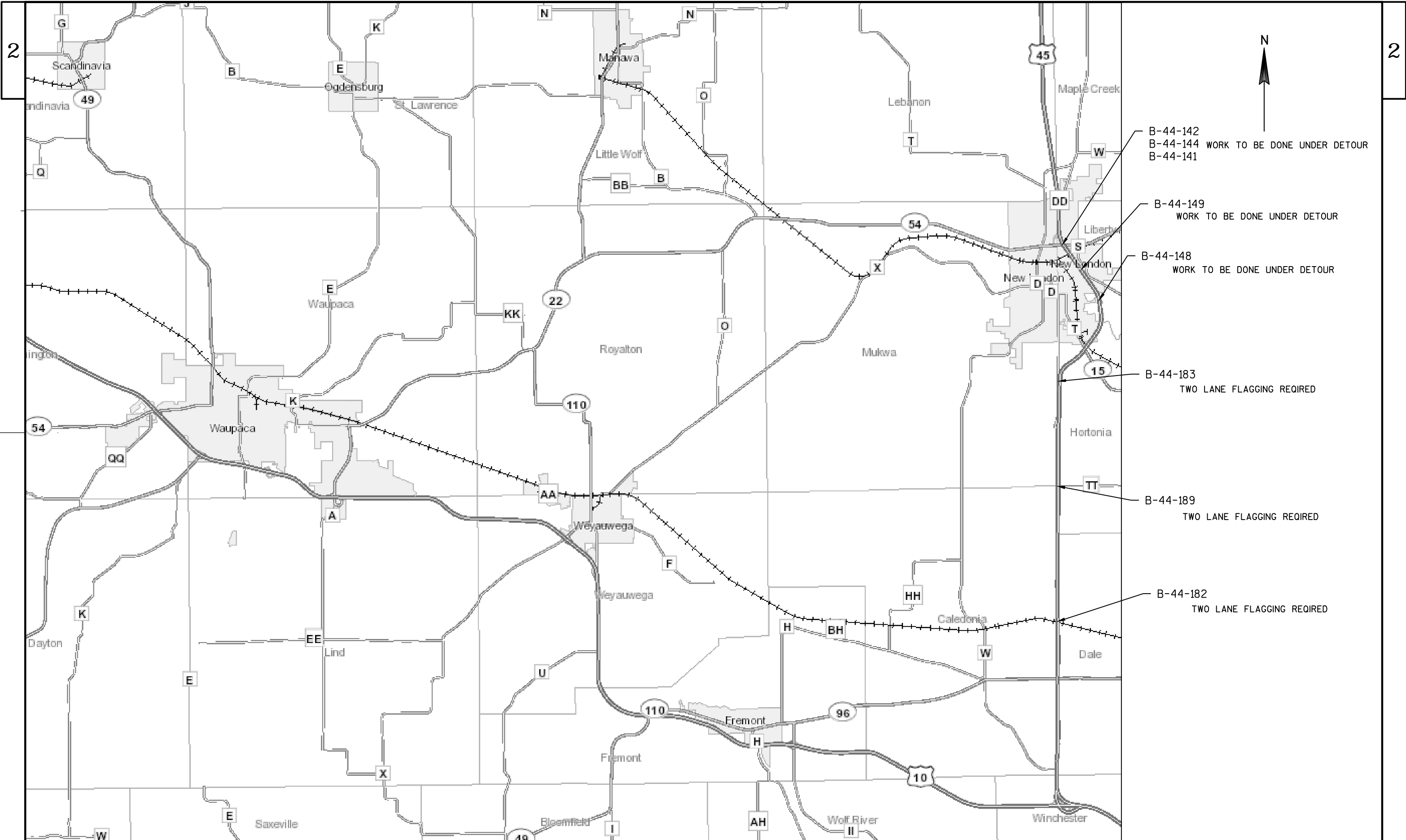
HWY: USH 10

COUNTY: WAUPACA

PLAN: OVERVIEW

SHEET

E



PROJECT NO:1009-42-64

HWY: USH 10

COUNTY: WAUPACA

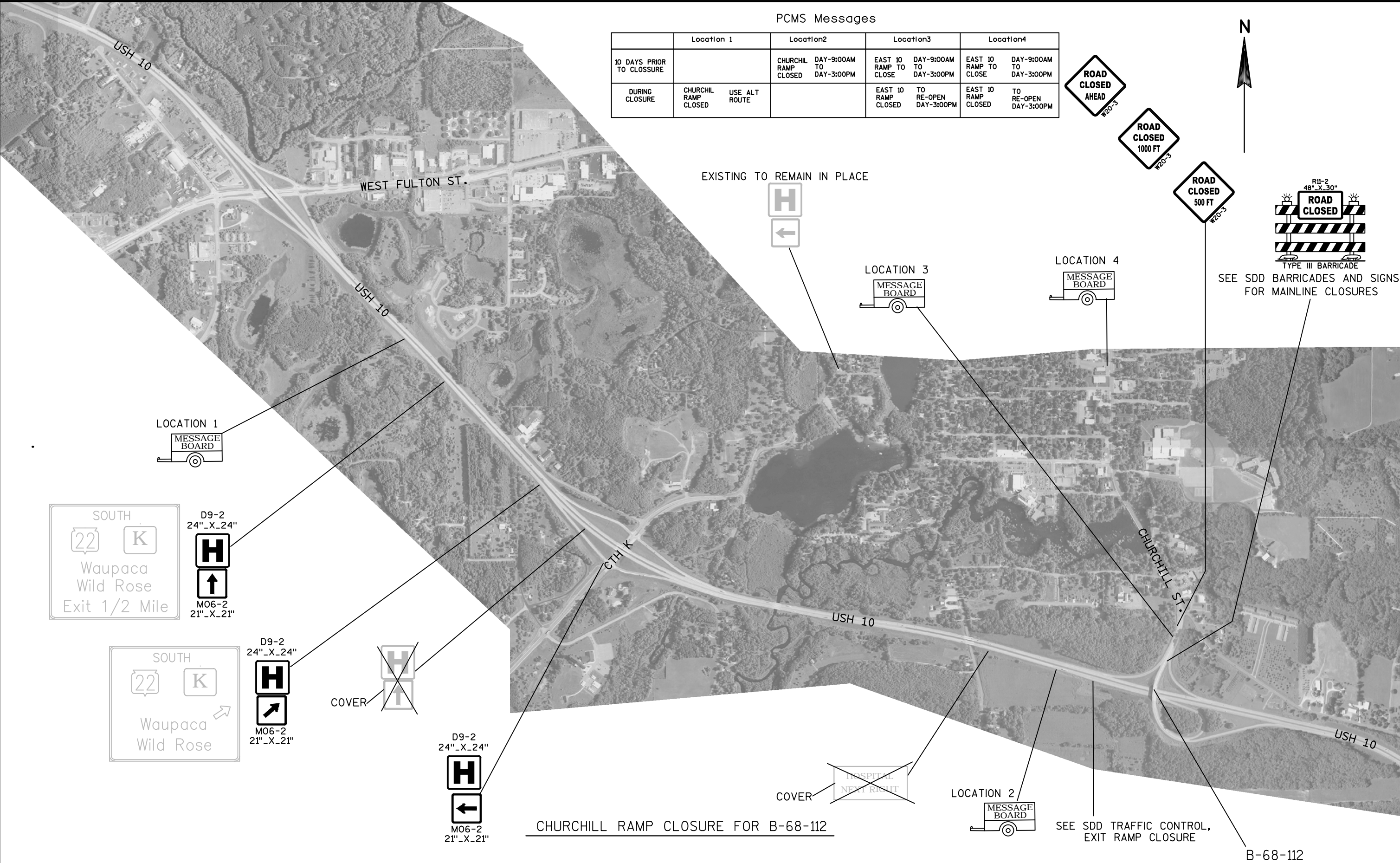
PLAN: OVERVIEW

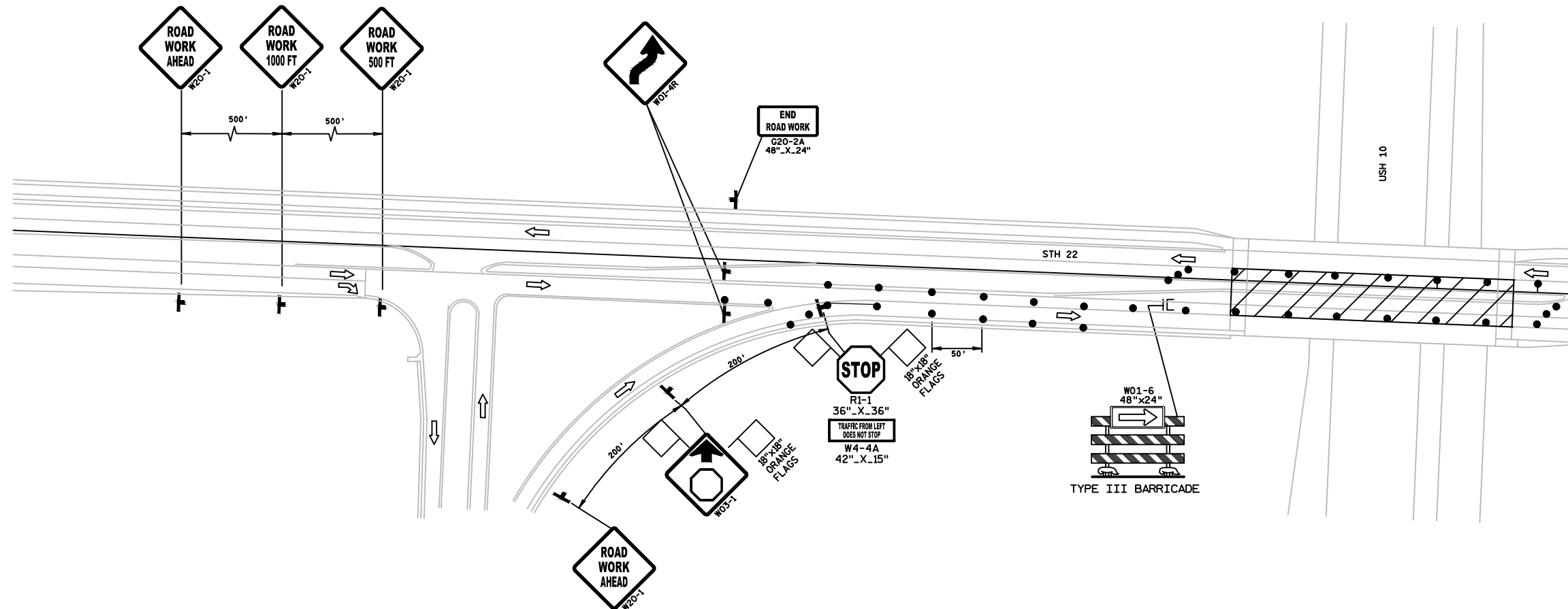
SHEET

E

PCMS Messages

	Location 1	Location2	Location3	Location4
10 DAYS PRIOR TO CLOSURE		CHURCHIL RAMP CLOSED DAY-9:00AM TO DAY-3:00PM	EAST 10 RAMP TO CLOSE DAY-9:00AM TO DAY-3:00PM	EAST 10 RAMP TO CLOSE DAY-9:00AM TO DAY-3:00PM
DURING CLOSURE	CHURCHIL RAMP CLOSED USE ALT ROUTE		EAST 10 RAMP CLOSED TO RE-OPEN DAY-3:00PM	EAST 10 RAMP CLOSED TO RE-OPEN DAY-3:00PM





LEGEND

- WORK ZONE
- FLAGGER WITH SLOW/STOP PADDLE
- SIGN ON PORTABLE SUPPORT
- TYPE III BARRICADE WITH SIGN
- DRUM
- DIRECTION OF TRAFFIC FLOW

TRAFFIC CONTROL NOTES

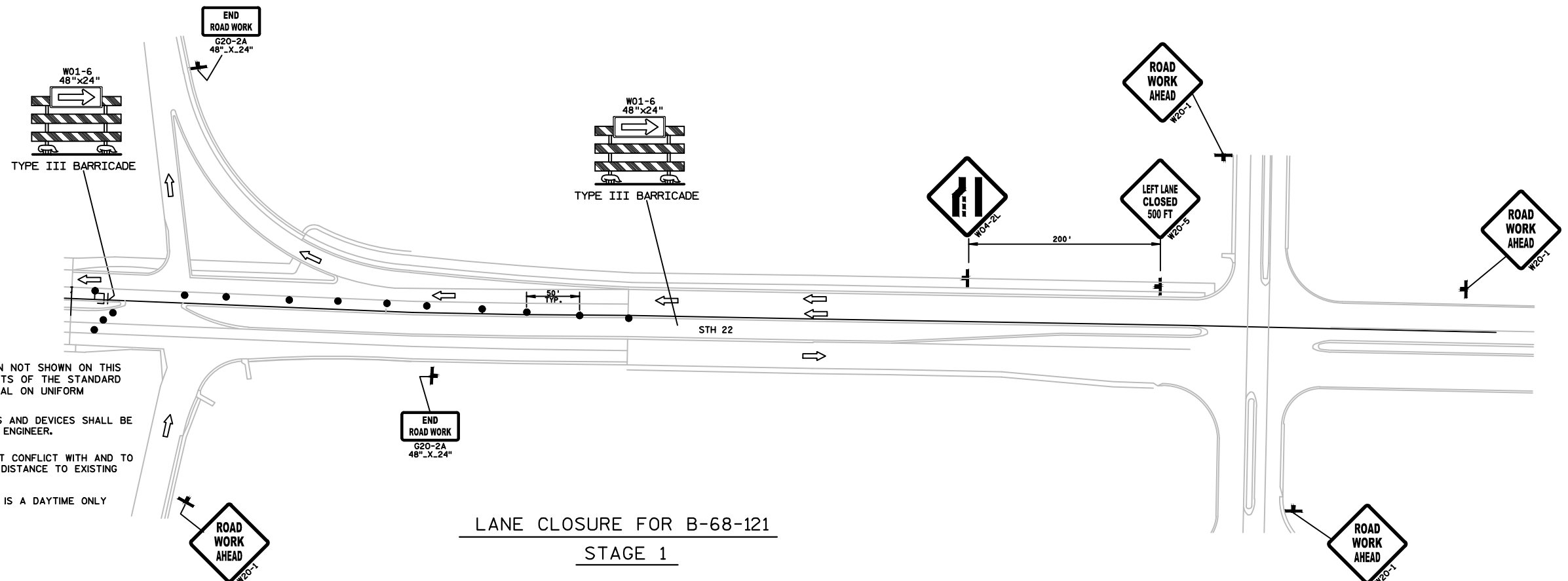
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

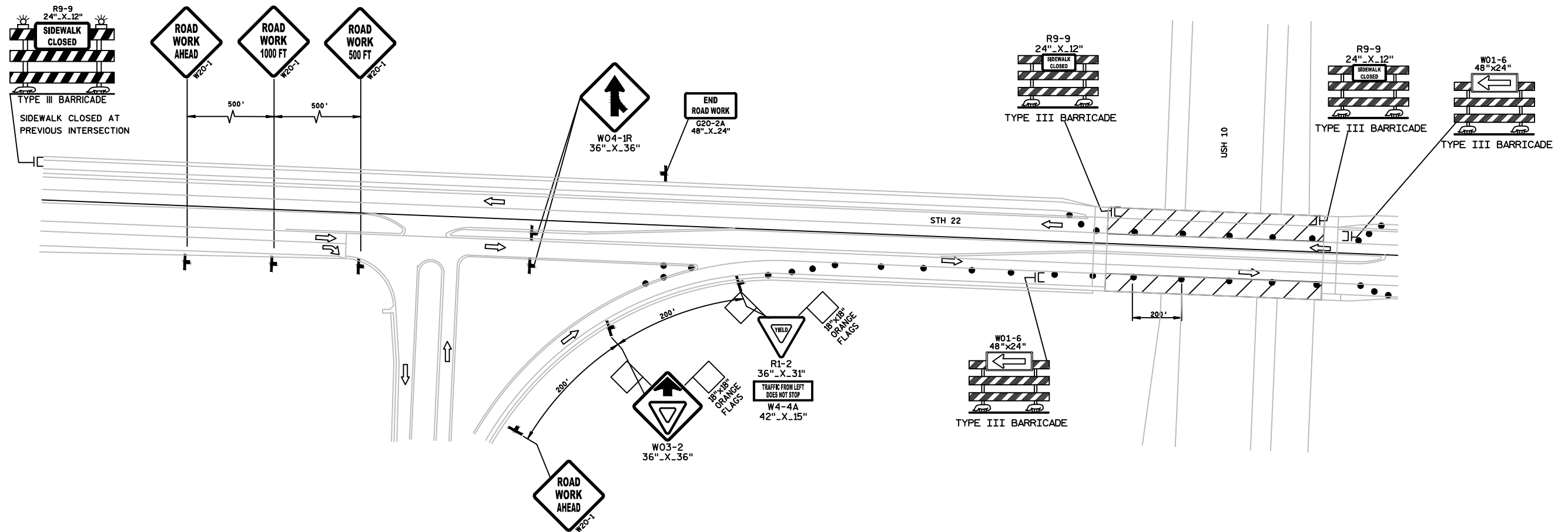
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING SIGNS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.



LANE CLOSURE FOR B-68-121
STAGE 1



LEGEND

- WORK ZONE
- FLAGGER WITH SLOW/STOP PADDLE
- SIGN ON PORTABLE SUPPORT
- TYPE III BARRICADE WITH SIGN
- DRUM
- DIRECTION OF TRAFFIC FLOW

TRAFFIC CONTROL NOTES

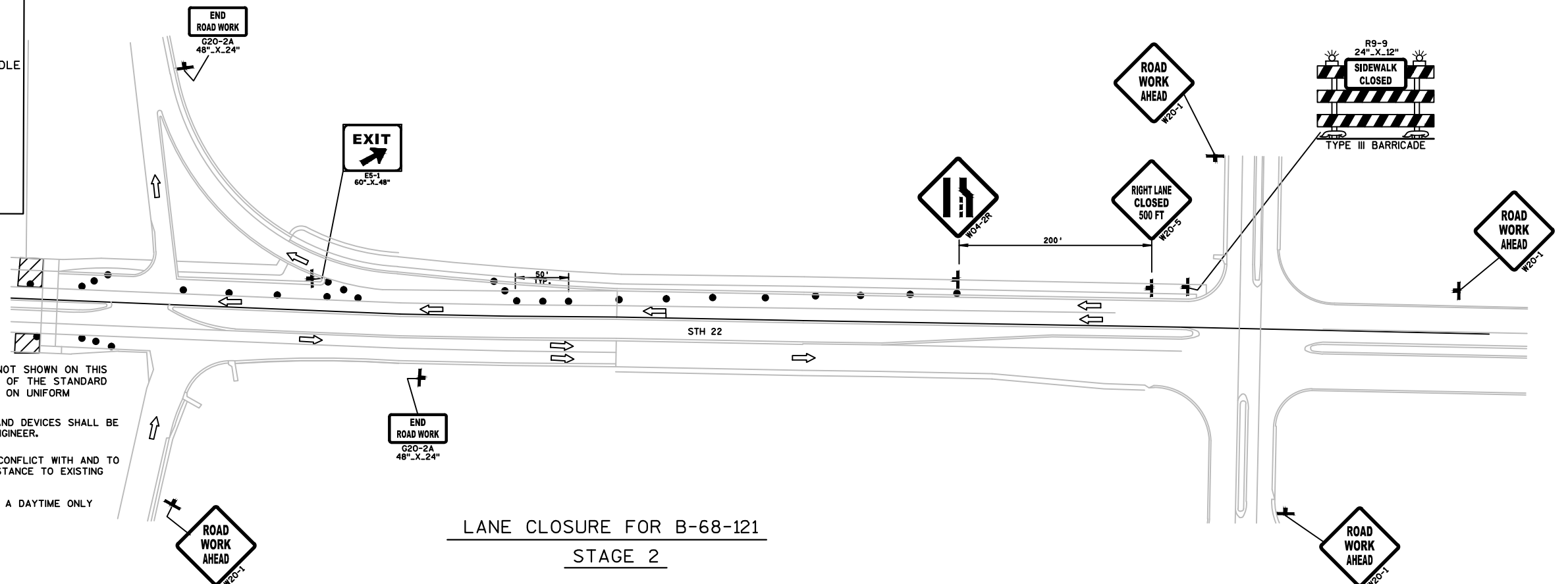
DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

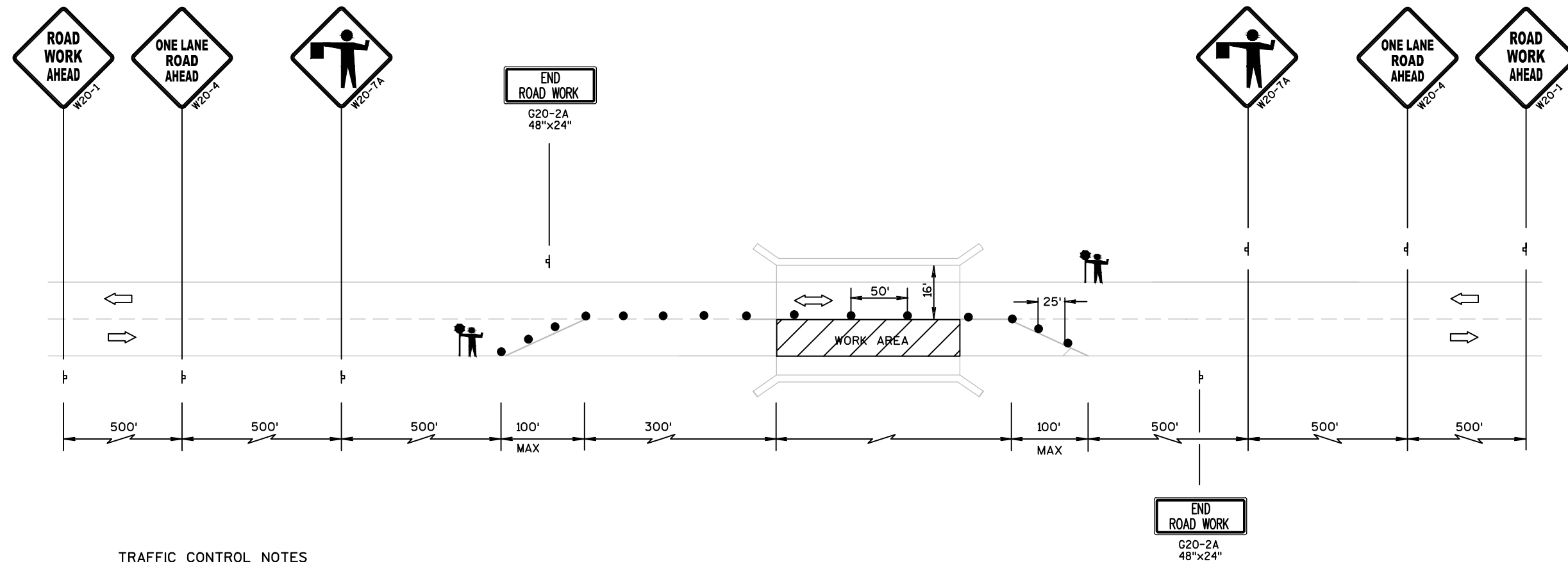
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING SIGNS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.



LANE CLOSURE FOR B-68-121
STAGE 2



TRAFFIC CONTROL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCED WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIREABLE) DISTANCE TO EXISTING SIGNS.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.







ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

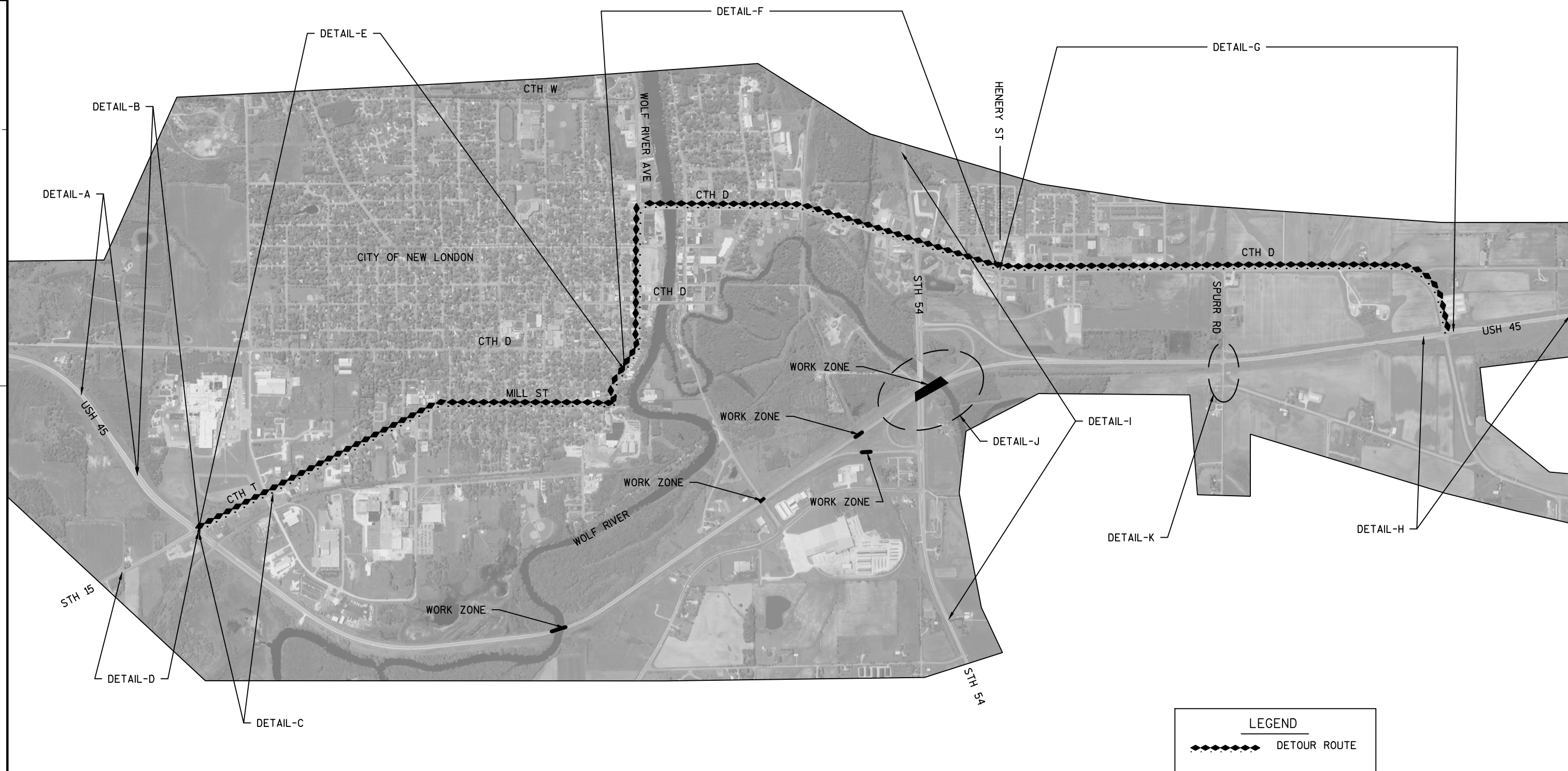
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS.

WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, THE "FLAGGER AHEAD", THE "ROAD WORK AHEAD" AND THE "ONE LANE ROAD AHEAD", SIGNS SHALL BE COVERED OR REMOVED AND THE HIGHWAY RESTORED TO NORMAL OPERATION.

TYPICAL LANE CLOSURE WITH FLAGGING SEE S.D.D 15C12 FOR MORE DETAILS

LEGEND

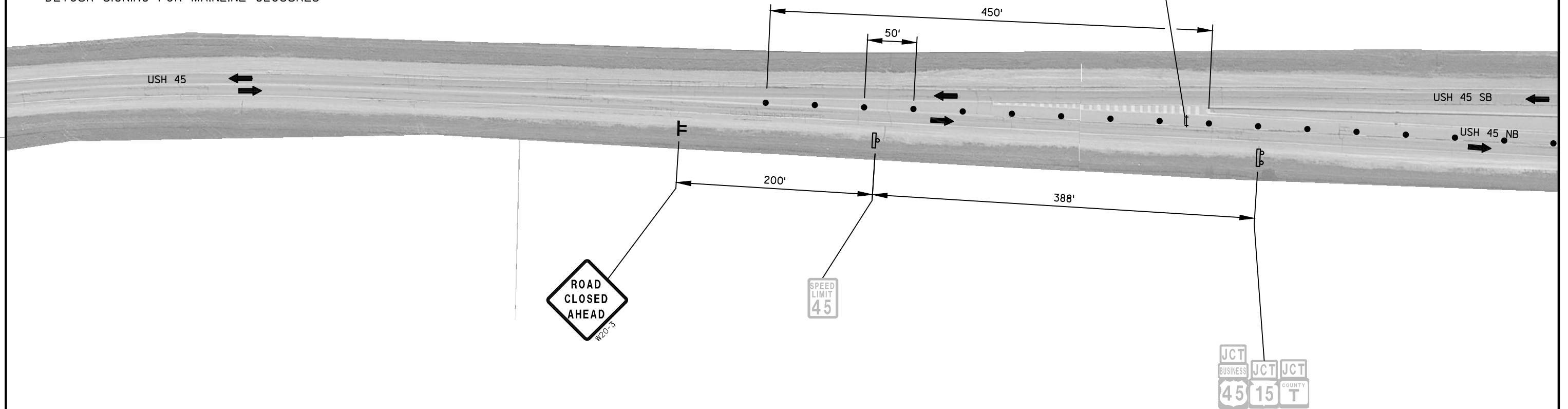
-  WORK ZONE
-  FLAGGER WITH SLOW/STOP PADDLE
-  SIGN ON PORTABLE SUPPORT
-  TYPE III BARRICADE WITH SIGN
-  DRUM
-  DIRECTION OF TRAFFIC FLOW



LEGEND

↑	TYPE III BARRICADE
↑	TYPE III BARRICADE WITH ATTACHED SIGN
•	TRAFFIC CONTROL DRUM
⚡	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
⏏	EXISTING SIGN ON PERMANENT SUPPORT
⏏	SIGN ON TEMPORARY SUPPORT
Ⓐ	TYPE A WARNING LIGHT (FLASHING)
Ⓑ	TYPE B WARNING LIGHT (HIGH INTENSITY FLASHING)
Ⓒ	TYPE C WARNING LIGHT (STEADY BURN)
→	DIRECTION OF TRAFFIC
MB	PORTABLE CHANGEABLE MESSAGE BOARD
▨	COVER EXISTING SIGN

SEE SDD:
BARRICADES AND SIGNS FOR MAINLINE CLOSURES
DETOUR SIGNING FOR MAINLINE CLOSURES



SEE SDD:
BARRICADES AND SIGNS FOR MAINLINE CLOSURES
DETOUR SIGNING FOR MAINLINE CLOSURES

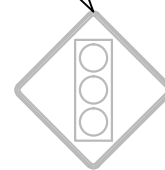
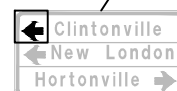
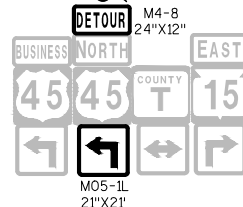
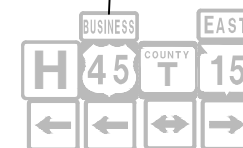
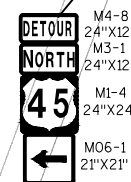
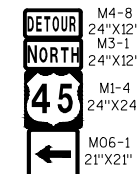
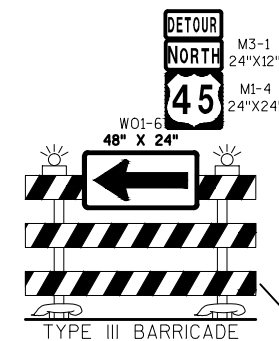
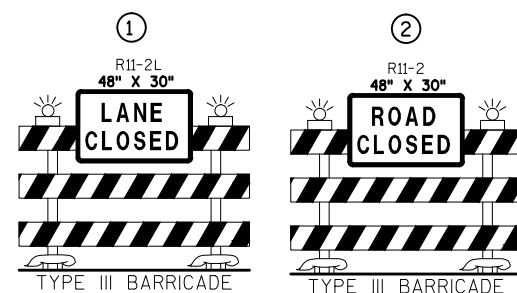
LEGEND

- † TYPE III BARRICADE
- † TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- † EXISTING SIGN ON PERMANENT SUPPORT
- † SIGN ON TEMPORARY SUPPORT
- Ⓐ TYPE A WARNING LIGHT (FLASHING)
- Ⓑ TYPE B WARNING LIGHT (HIGH INTENSITY FLASHING)
- Ⓒ TYPE C WARNING LIGHT (STEADY BURN)
- ➔ DIRECTION OF TRAFFIC
- MB PORTABLE CHANGEABLE MESSAGE BOARD
- COVER EXISTING SIGN

MESSAGE BOARD
USH 45 NORTH
(200 FEET NORTH OF STH 15/CTH T
INTERSECTION)

PHASE 1	PHASE 2
ROAD TO CLOSE	MONDAY JUNE XX 9:00 AM

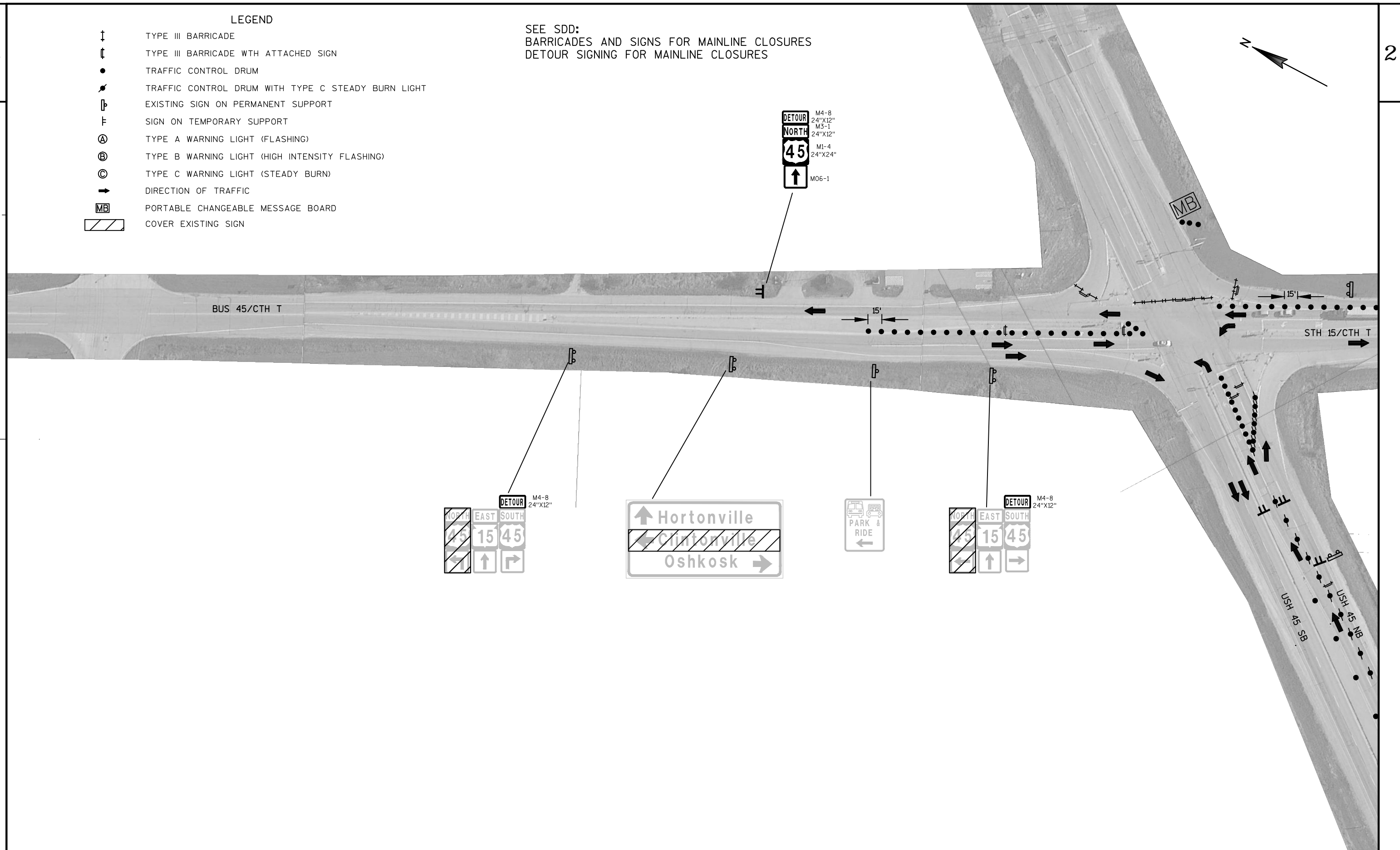
INSTALL 1 WEEK PRIOR TO DETOUR

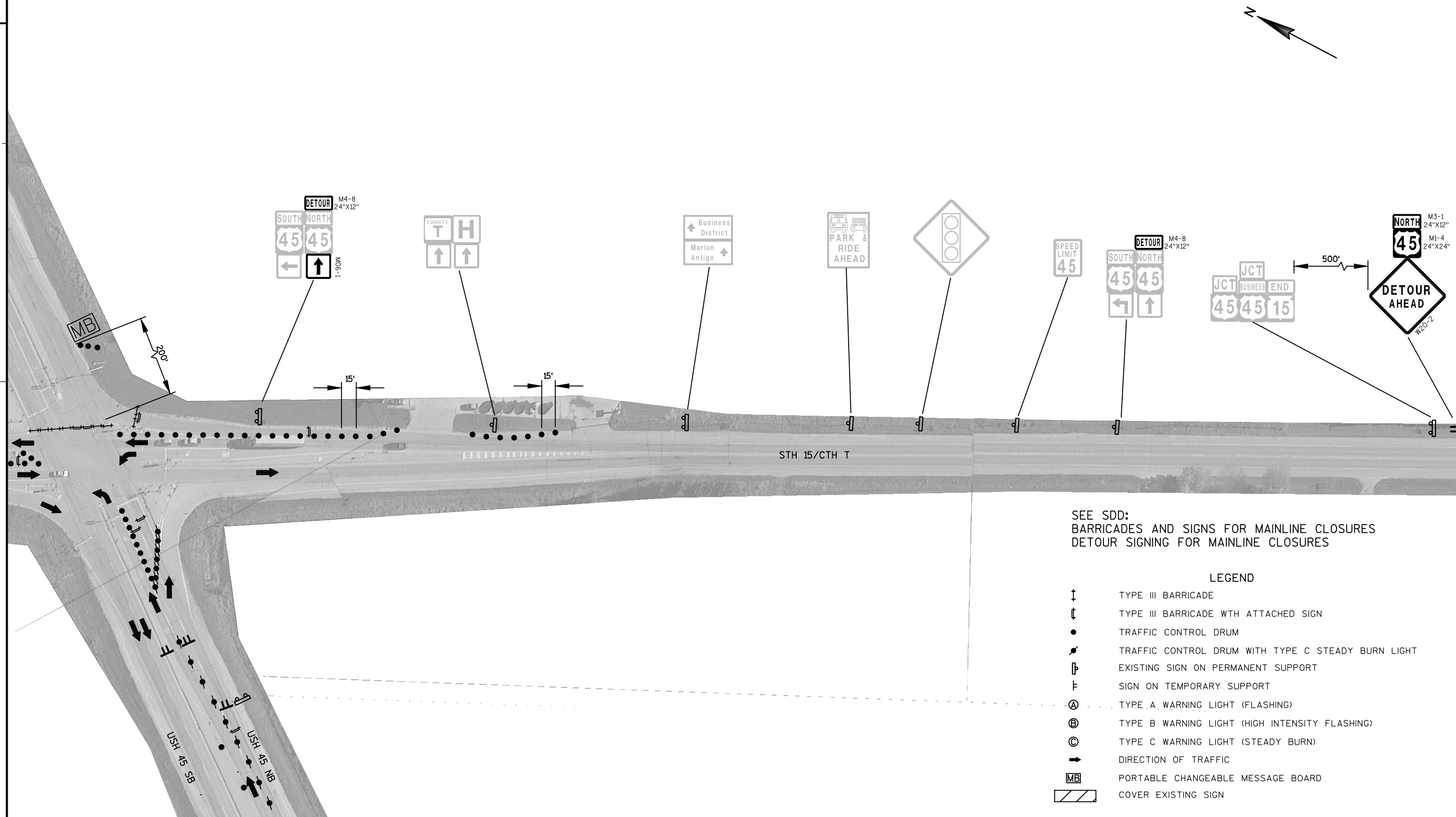


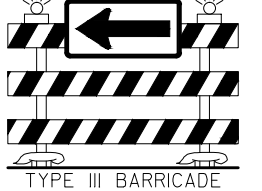
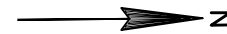
LEGEND

↑	TYPE III BARRICADE
↑	TYPE III BARRICADE WTH ATTACHED SIGN
●	TRAFFIC CONTROL DRUM
●	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
⏏	EXISTING SIGN ON PERMANENT SUPPORT
⏏	SIGN ON TEMPORARY SUPPORT
Ⓐ	TYPE A WARNING LIGHT (FLASHING)
Ⓑ	TYPE B WARNING LIGHT (HIGH INTENSITY FLASHING)
Ⓒ	TYPE C WARNING LIGHT (STEADY BURN)
→	DIRECTION OF TRAFFIC
MB	PORTABLE CHANGEABLE MESSAGE BOARD
▨	COVER EXISTING SIGN

SEE SDD:
BARRICADES AND SIGNS FOR MAINLINE CLOSURES
DETOUR SIGNING FOR MAINLINE CLOSURES



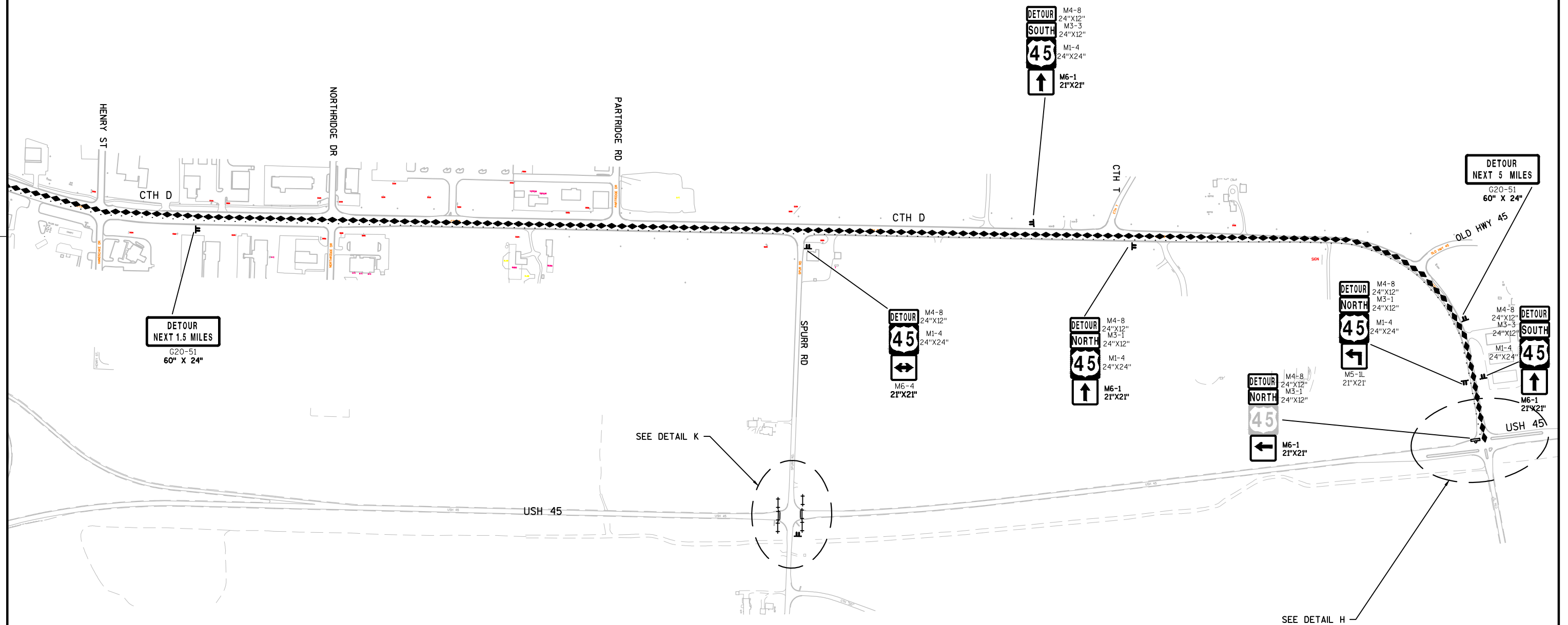


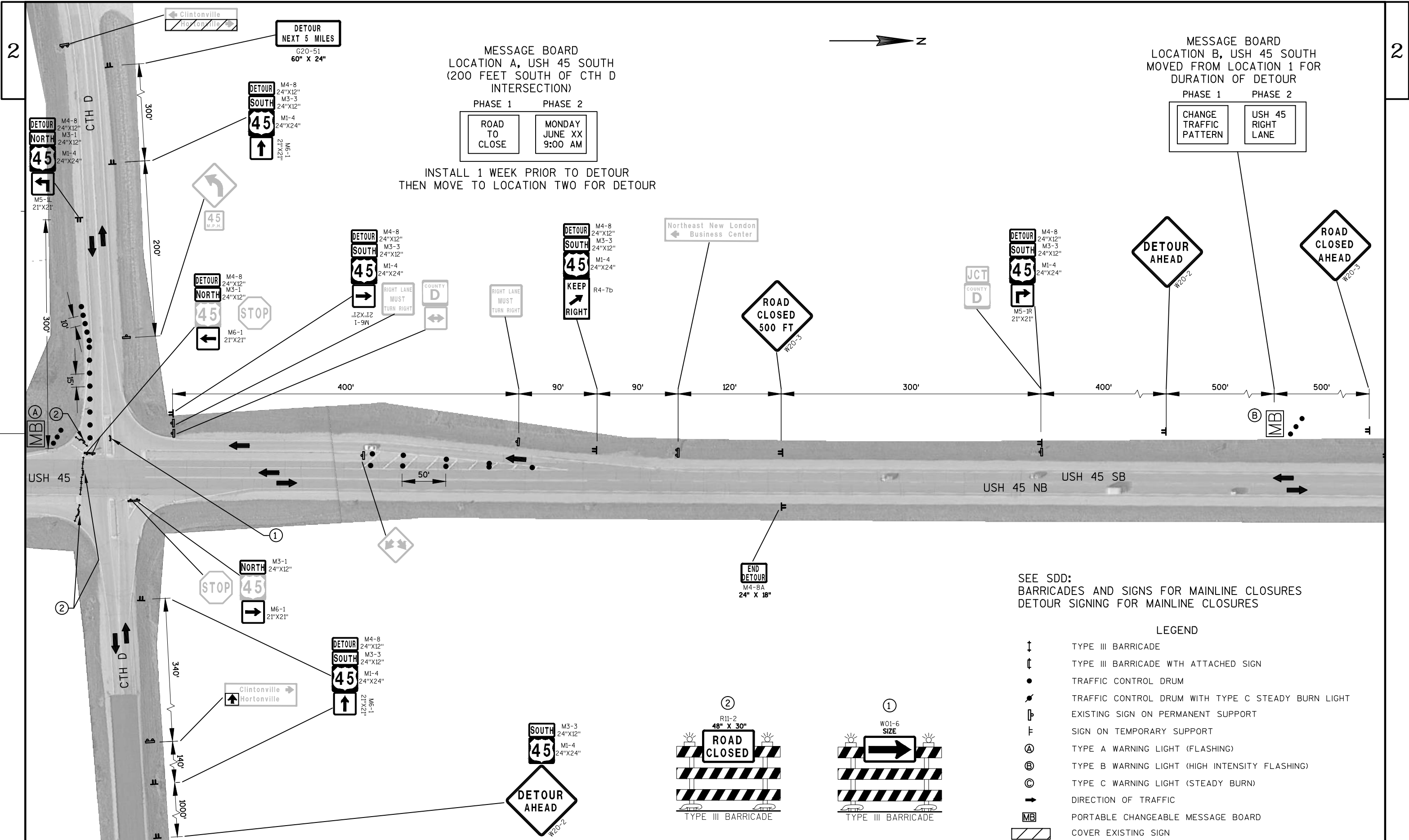




LEGEND

- ◆◆◆◆◆◆◆◆◆◆ DETOUR ROUTE
- ⌄ TYPE III BARRICADE WITH ATTACHED SIGN
- ⌄ TYPE III BARRICADE
- ⌄ SIGN ON TEMPORARY SUPPORT
- ⌄ EXISTING SIGN ON PERMANENT SUPPORT





MESSAGE BOARD
LOCATION A, USH 45 SOUTH
(200 FEET SOUTH OF CTH D
INTERSECTION)

PHASE 1	PHASE 2
ROAD TO CLOSE	MONDAY JUNE XX 9:00 AM

INSTALL 1 WEEK PRIOR TO DETOUR
THEN MOVE TO LOCATION TWO FOR DETOUR

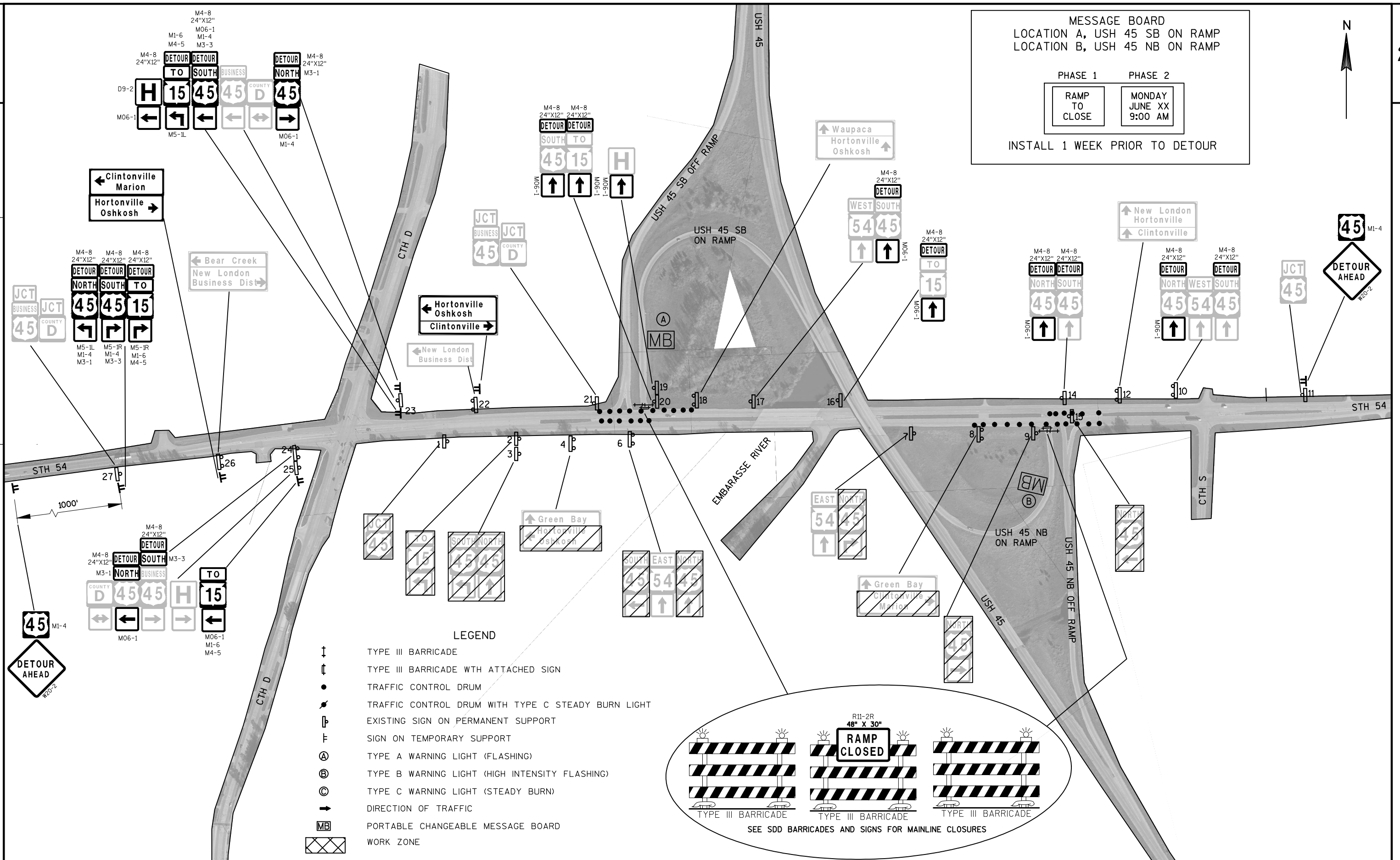
MESSAGE BOARD
LOCATION B, USH 45 SOUTH
MOVED FROM LOCATION 1 FOR
DURATION OF DETOUR

PHASE 1	PHASE 2
CHANGE TRAFFIC PATTERN	USH 45 RIGHT LANE

SEE SDD:
BARRICADES AND SIGNS FOR MAINLINE CLOSURES
DETOUR SIGNING FOR MAINLINE CLOSURES

LEGEND

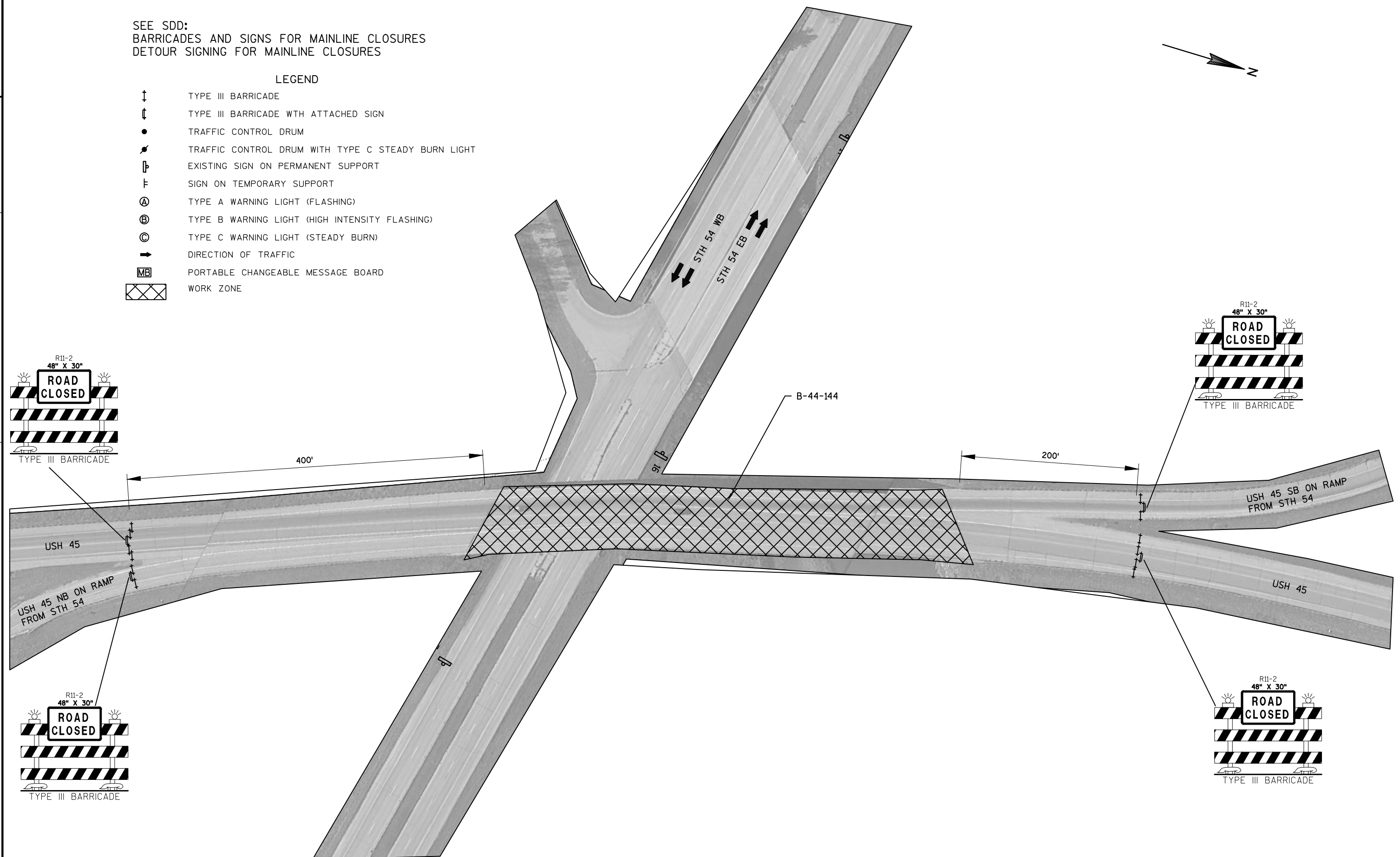
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- EXISTING SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- TYPE A WARNING LIGHT (FLASHING)
- TYPE B WARNING LIGHT (HIGH INTENSITY FLASHING)
- TYPE C WARNING LIGHT (STEADY BURN)
- DIRECTION OF TRAFFIC
- PORTABLE CHANGEABLE MESSAGE BOARD
- COVER EXISTING SIGN

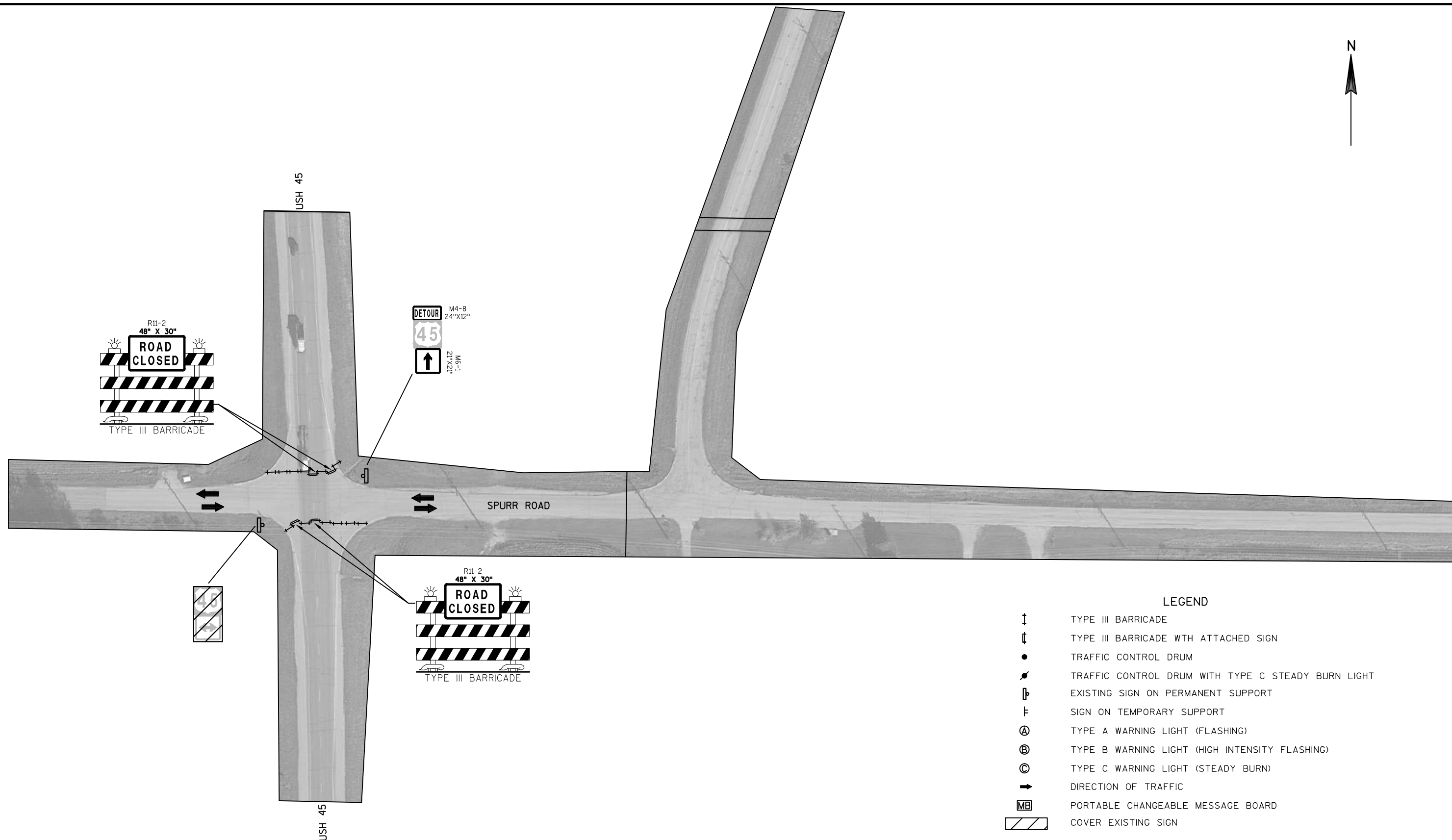


SEE SDD:
BARRICADES AND SIGNS FOR MAINLINE CLOSURES
DETOUR SIGNING FOR MAINLINE CLOSURES

LEGEND

- ↑ TYPE III BARRICADE
- ↑ TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- ⊥ EXISTING SIGN ON PERMANENT SUPPORT
- ⊥ SIGN ON TEMPORARY SUPPORT
- Ⓐ TYPE A WARNING LIGHT (FLASHING)
- Ⓑ TYPE B WARNING LIGHT (HIGH INTENSITY FLASHING)
- Ⓒ TYPE C WARNING LIGHT (STEADY BURN)
- DIRECTION OF TRAFFIC
- MB PORTABLE CHANGEABLE MESSAGE BOARD
- ⊠ WORK ZONE





SEE SDD:
BARRICADES AND SIGNS FOR MAINLINE CLOSURES
DETOUR SIGNING FOR MAINLINE CLOSURES

DATE 11FEB15				E S T I M A T E O F Q U A N T I T I E S			
LINE						1009-42-62	1009-42-64
NUMBER	ITEM	ITEM DESCRIPTION	UNIT	TOTAL	QUANTITY	QUANTITY	QUANTITY
0010	502.3215.S	Protective Surface Treatment Reseal	SY	37,657.000	31,824.000	5,833.000	
0020	509.5100.S	Polymer Overlay	SY	4,086.000		4,086.000	
0030	619.1000	Mobilization	EACH	1.000	0.350	0.650	
0040	643.0100	Traffic Control (project) 01.	EACH	1.000	1.000		
0050	643.0100	Traffic Control (project) 02.	EACH	1.000		1.000	
0060	643.0300	Traffic Control Drums	DAY	3,613.000	3,058.000	555.000	
0070	643.0420	Traffic Control Barricades Type III	DAY	402.000	169.000	233.000	
0080	643.0705	Traffic Control Warning Lights Type A	DAY	496.000		496.000	
0090	643.0715	Traffic Control Warning Lights Type C	DAY	25.000		25.000	
0100	643.0800	Traffic Control Arrow Boards	DAY	216.000	216.000		
0110	643.0900	Traffic Control Signs	DAY	1,191.000	1,011.000	180.000	
0120	643.0920	Traffic Control Covering Signs Type II	EACH	23.000	6.000	17.000	
0130	643.1050	Traffic Control Signs PCMS	DAY	188.000	100.000	88.000	
0140	643.2000	Traffic Control Detour (project) 01.	EACH	1.000		1.000	
0150	643.3000	Traffic Control Detour Signs	DAY	702.000		702.000	
0160	646.0106	Pavement Marking Epoxy 4-Inch	LF	2,400.000		2,400.000	
0170	646.0126	Pavement Marking Epoxy 8-Inch	LF	1,650.000		1,650.000	
0180	SPV.0060	Special 01. Crack Sealing Epoxy	EACH	33.000	26.000	7.000	

PROTECTIVE SURFACE TREATMENT RESEAL AND CRACK
SEALING EPOXY

502.3215.S PROTECTIVE SURFACE TREATMENT RESEAL SY				SPV.0060.01 CRACK SEALING EPOXY EA		
CATTEGORY	BRIDGE	1009-42-62	1009-42-64	1009-42-62	1009-42-64	REMARKS
10	B-68-19	1284	-	1	-	USH 10
10	B-68-21	360	-	1	-	USH 10
10	B-68-22	662	-	1	-	USH 10
10	B-68-73	1280	-	1	-	USH 10
10	B-68-74	929	-	1	-	USH 10
10	B-68-75	364	-	1	-	USH 10
10	B-68-76	661	-	1	-	USH 10
10	B-68-77	932	-	1	-	USH 10
10	B-68-83	819	-	1	-	USH 10
10	B-68-84	817	-	1	-	USH 10
10	B-68-89	4682	-	1	-	USH 10
10	B-68-90	4671	-	1	-	USH 10
10	B-68-92	1167	-	1	-	USH 10
10	B-68-93	1202	-	1	-	USH 10
10	B-68-94	604	-	1	-	USH 10
10	B-68-95	602	-	1	-	USH 10
10	B-68-96	752	-	1	-	USH 10
10	B-68-97	755	-	1	-	USH 10
10	B-68-98	1014	-	1	-	USH 10
10	B-68-99	1005	-	1	-	USH 10
10	B-68-102	401	-	1	-	USH 10
10	B-68-103	612	-	1	-	USH 10
10	B-68-104	143	-	0	-	USH 10
10	B-68-105	149	-	0	-	USH 10
10	B-68-109	564	-	1	-	USH 10
10	B-68-110	557	-	1	-	USH 10
10	B-68-112	1445	-	1	-	USH 10
10	B-68-121	3391	-	1	-	USH 10
10	B-44-141	-	809	-	1	USH 45
10	B-44-142		545	-	1	USH 45
10	B-44-144	-	582	-	0	USH 45
10	B-44-148	-	1171	-	1	USH 45
10	B-44-149	-	611	-	1	USH 45
10	B-44-182	-	786	-	1	USH 45
10	B-44-183	-	711	-	1	USH 45
10	B-44-189	-	618	-	1	USH 45
Sub Total		31,824	5,833	26	7	
Total		37,657		33		

TRAFFIC CONTROL SIGNS PORTABLE CHANGEABLE MESSAGE

				643.1050	643.1050	
		NUMBER	DAYS	DAYS	DAYS	
CATTEGORY	LOCATION	REQUIRED	NEEDED	1009-42-62	1009-42-64	REMARKS
10	B-68-112	4	15	60	-	CHURCHILL
10	USH 10	1	20	20	-	WESTBOUND
10	USH 10	1	20	20	-	EASTBOUND
20	USH 45	1	10	-	10	NORTHBOUND
20	USH 45	1	10	-	10	SOUTHBOUND
20	USH 45	4	34		68	DETOUR
SUBTOTAL				100	20	
CATEGORY 20 TOTAL				0	68	
TOTAL				188		

PAVMENT MARKING EPOXY

		L.F.	L.F.		
		646.0106	646.0106	L.F.	
		4"	4"	646.0126	
CATTEGORY	LOCATION	White	Yellow	8"	REMARKS
20	B-44-144	1150	1250	1650	USH 45
		1150	1250	1650	
TOTAL		2400		1650	

TRAFFIC CONTROL

		643.2000.		643.0100	
		TRAFFIC CONTROL		TRAFFIC CONTROL	
		DETOUR		PROJECT	
CATTEGORY	PROJECT			EA	REMARKS
10	1009-42-62			1	USH 10
20	1009-42-64	1			USH 45 B-44-144
10	1009-42-64			1	USH 45
CATEGORY 20		TOTAL	1	0	
		TOTAL	1	2	

TRAFFIC CONTROL DRUMS

CATEGORY	LOCATION	NUMBER REQUIRED	DAYS NEEDED	643.0300 DAYS		REMARKS
				1009-42-62	1009-42-64	
10	B-68-19	25	4	100	-	USH 10
10	B-68-21	25	4	100	-	USH 10
10	B-68-22	25	4	100	-	USH 10
10	B-68-25	25	4	100	-	USH 10
10	B-68-73	25	4	100	-	USH 10
10	B-68-74	25	4	100	-	USH 10
10	B-68-75	25	4	100	-	USH 10
10	B-68-76	25	4	100	-	USH 10
10	B-68-77	25	4	100	-	USH 10
10	B-68-83	25	4	100	-	USH 10
10	B-68-84	25	4	100	-	USH 10
10	B-68-89	25	4	100	-	USH 10
10	B-68-90	25	4	100	-	USH 10
10	B-68-92	25	4	100	-	USH 10
10	B-68-93	25	4	100	-	USH 10
10	B-68-94	25	4	100	-	USH 10
10	B-68-95	25	4	100	-	USH 10
10	B-68-96	25	4	100	-	USH 10
10	B-68-97	25	4	100	-	USH 10
10	B-68-98	25	4	100	-	USH 10
10	B-68-99	25	4	100	-	USH 10
10	B-68-102	25	4	100	-	USH 10
10	B-68-103	25	4	100	-	USH 10
10	B-68-104	25	4	100	-	USH 10
10	B-68-105	25	4	100	-	USH 10
10	B-68-109	25	4	100	-	USH 10
10	B-68-110	25	4	100	-	USH 10
10	B-68-112	30	5	150	-	CHURCHILL
10	B-68-121	52	4	208	-	STH 22
20	B-44-141,142,144	50	4	-	155	USH 45
10	B-44-148	20	4	-	80	USH 45
10	B-44-149	20	4	-	80	USH 45
10	B-44-182	20	4	-	80	USH 45
10	B-44-183	20	4	-	80	USH 45
10	B-44-189	20	4	-	80	USH 45
SUB TOTAL				3058	400	
CATEGORY 20 TOTAL				0	155	
TOTAL				3613		

TRAFFIC CONTROL BARRICADES TYPE III

CATEGORY	LOCATION	NUMBER REQUIRED	DAYS NEEDED	643.0420 DAYS		REMARKS
				1009-42-62	1009-42-64	
10	B-68-19	1	4	4	-	USH 10
10	B-68-21	1	4	4	-	USH 10
10	B-68-22	1	4	4	-	USH 10
10	B-68-25	1	4	4	-	USH 10
10	B-68-73	1	4	4	-	USH 10
10	B-68-74	1	4	4	-	USH 10
10	B-68-75	1	4	4	-	USH 10
10	B-68-76	1	4	4	-	USH 10
10	B-68-77	1	4	4	-	USH 10
10	B-68-83	1	4	4	-	USH 10
10	B-68-84	1	4	4	-	USH 10
10	B-68-89	1	4	4	-	USH 10
10	B-68-90	1	4	4	-	USH 10
10	B-68-92	1	4	4	-	USH 10
10	B-68-93	1	4	4	-	USH 10
10	B-68-94	1	4	4	-	USH 10
10	B-68-95	1	4	4	-	USH 10
10	B-68-96	1	4	4	-	USH 10
10	B-68-97	1	4	4	-	USH 10
10	B-68-98	1	4	4	-	USH 10
10	B-68-99	1	4	4	-	USH 10
10	B-68-102	1	4	4	-	USH 10
10	B-68-103	1	4	4	-	USH 10
10	B-68-104	1	4	4	-	USH 10
10	B-68-105	1	4	4	-	USH 10
10	B-68-109	1	4	4	-	USH 10
10	B-68-110	1	4	4	-	USH 10
10	B-68-112	9	5	45	-	CHURCHILL
10	B-68-121	4	4	16	-	STH 22
20	B-44-141,142,144	71	3	-	213	USH 45
10	B-44-148	1	4	-	4	USH 45
10	B-44-149	1	4	-	4	USH 45
10	B-44-182	1	4	-	4	USH 45
10	B-44-183	1	4	-	4	USH 45
10	B-44-189	1	4	-	4	USH 45
SUB TOTAL				169	20	
CATEGORY 20 TOTAL					213	
TOTAL				402		

TRAFFIC CONTROL

		643.0715	643.0705	REMARKS
		WARNING	WARNING	
		LIGHTS	LIGHTS	
		TYPE C	TYPE A	
CATTEGORY	LOCATION	DAYS	DAYS	
		1009-42-64	1009-42-64	
10	B-68-19	-	-	USH 10
10	B-68-21	-	-	USH 10
10	B-68-22	-	-	USH 10
10	B-68-25	-	-	USH 10
10	B-68-73	-	-	USH 10
10	B-68-74	-	-	USH 10
10	B-68-75	-	-	USH 10
10	B-68-76	-	-	USH 10
10	B-68-77	-	-	USH 10
10	B-68-83	-	-	USH 10
10	B-68-84	-	-	USH 10
10	B-68-89	-	-	USH 10
10	B-68-90	-	-	USH 10
10	B-68-92	-	-	USH 10
10	B-68-93	-	-	USH 10
10	B-68-94	-	-	USH 10
10	B-68-95	-	-	USH 10
10	B-68-96	-	-	USH 10
10	B-68-97	-	-	USH 10
10	B-68-98	-	-	USH 10
10	B-68-99	-	-	USH 10
10	B-68-102	-	-	USH 10
10	B-68-103	-	-	USH 10
10	B-68-104	-	-	USH 10
10	B-68-105	-	-	USH 10
10	B-68-109	-	-	USH 10
10	B-68-110	-	-	USH 10
10	B-68-112	-	-	CHURCHILL
10	B-68-121	-	-	STH 22
20	B-44-141,142,144	25	496	USH 45
10	B-44-148	-	-	USH 45
10	B-44-149	-	-	USH 45
10	B-44-182	-	-	USH 45
10	B-44-183	-	-	USH 45
10	B-44-189	-	-	USH 45
SUB TOTALS		0	-	
CATTEGORY 20 TOTALS		25	496	
TOTALS		25	496	

TRAFFIC CONTROL ARROW BOARDS

				643.0800	REMARKS
				DAYS	
				DAYS	
				1009-42-62	
CATTEGORY	LOCATION	NUMBER	DAYS		
		REQUIRED	NEEDED		
10	B-68-19	2	4	8	USH 10
10	B-68-21	2	4	8	USH 10
10	B-68-22	2	4	8	USH 10
10	B-68-25	2	4	8	USH 10
10	B-68-73	2	4	8	USH 10
10	B-68-74	2	4	8	USH 10
10	B-68-75	2	4	8	USH 10
10	B-68-76	2	4	8	USH 10
10	B-68-77	2	4	8	USH 10
10	B-68-83	2	4	8	USH 10
10	B-68-84	2	4	8	USH 10
10	B-68-89	2	4	8	USH 10
10	B-68-90	2	4	8	USH 10
10	B-68-92	2	4	8	USH 10
10	B-68-93	2	4	8	USH 10
10	B-68-94	2	4	8	USH 10
10	B-68-95	2	4	8	USH 10
10	B-68-96	2	4	8	USH 10
10	B-68-97	2	4	8	USH 10
10	B-68-98	2	4	8	USH 10
10	B-68-99	2	4	8	USH 10
10	B-68-102	2	4	8	USH 10
10	B-68-103	2	4	8	USH 10
10	B-68-104	2	4	8	USH 10
10	B-68-105	2	4	8	USH 10
10	B-68-109	2	4	8	USH 10
10	B-68-110	2	4	8	USH 10
TOTAL				216	

TRAFFIC CONTROL SIGNS
(FOR INFORMATIONAL PURPOSES)

LOCATION	SIGN CODE	MESSAGE	NUMBER REQUIRED	DAYS NEEDED	SIGN DAYS	REMARKS
SINGLE LANE CLOSURE	W20-1	ROAD WORK AHEAD	1	4	4	1009-42-62
	W20-1	ROAD WORK 1 MILE	1	4	4	USH 10
	W20-5	RT/LT LANE CLOSED 1/2 MILE	1	4	4	
	W20-5	RT/LT LANE CLOSED 1500 FT	1	4	4	
	WO4-2R/L	LANE REDUCTION	1	4	4	
	WO1-4R/L	LT/RT REVERSE CURVE	2	4	8	
	G20-2A	END ROAD WORK	1	4	4	
LANE CLOSURE WITH FLAGGING	W20-1	ROAD WORK AHEAD	2	4	8	1009-42-64
	W20-4	ONE LANE ROAD 1500 FT	2	4	8	USH 45
	W3-4	BE PREPARED TO STOP	2	4	8	
	W20-7A	FLAGMAN	2	4	8	
	W01-6	NIGHT ARROW	1	4	4	
	G20-2A	END ROAD WORK	2	4	8	
B-68-112	W20-3	ROAD CLOSED AHEAD	1	5	5	1009-42-62
						CHURCHILL
	W20-3	ROAD CLOSED 1000 FT	1	5	5	RAMP
	W20-3	ROAD CLOSED 500 FT	1	5	5	
	R11-2	ROAD CLOSED	1	5	5	
	W20-1	ROAD WORK AHEAD	1	5	5	
	W20-3 (MOD.)	RAMP CLOSED AHEAD	1	5	5	
	W20-3 (MOD.)	RAMP CLOSED 1000FT	1	5	5	
	R11-2 (MOD.)	RAMP CLOSED	2	5	10	
	D9-2	H	3	5	15	
	M6-1	ARROW	2	5	10	
	MO6-2	DIAGONAL ARROW	1	5	5	
	G20-2A	END ROAD WORK	2	4	8	
B-68-121	W20-1	ROAD WORK AHEAD	6	4	24	1009-42-62
	W20-1	ROAD WORK 1000 FT	1	4	4	STH 22
	W20-1	ROAD WORK 500 FT	1	4	4	
	W20-5	RT/LT LANE CLOSED 500 FT	1	4	4	
	WO4-2R/L	LANE REDUCTION	1	4	4	
	WO1-4R	RT REVERSE CURVE	2	2	4	
	WO1-6	NIGHT ARROW	2	4	8	
	W3-1	STOP AHEAD	1	2	2	
	R1-1	STOP	1	2	2	
	W4-4A	AFFIC FROM LT DOES NOT STOP	1	4	4	
	W3-2	YEILD AHEAD	1	2	2	
	R1-2	YIELD	1	2	2	
	R9-9	SIDEWALK CLOSED	4	2	8	
	WO4-1R	RAMP MERGE RT	1	2	2	
	E5-1	EXT	1	2	2	
	G20-2A	END ROAD WORK	3	4	12	

TRAFFIC CONTROL								
CATTEGORY	LOCATION	643.0900		NUMBER OF CYCLES	643.0920		643.0920	
		SIGNS			COVERING SIGNS EA	NUMBER OF CYCLES		COVERING SIGNS EA
		1009-42-62	1009-42-64					
10	B-68-19	32	-	-	-	-	-	
10	B-68-21	32	-	-	-	-	-	
10	B-68-22	32	-	-	-	-	-	
10	B-68-25	32	-	-	-	-	-	
10	B-68-73	32	-	-	-	-	-	
10	B-68-74	32	-	-	-	-	-	
10	B-68-75	32	-	-	-	-	-	
10	B-68-76	32	-	-	-	-	-	
10	B-68-77	32	-	-	-	-	-	
10	B-68-83	32	-	-	-	-	-	
10	B-68-84	32	-	-	-	-	-	
10	B-68-89	32	-	-	-	-	-	
10	B-68-90	32	-	-	-	-	-	
10	B-68-92	32	-	-	-	-	-	
10	B-68-93	32	-	-	-	-	-	
10	B-68-94	32	-	-	-	-	-	
10	B-68-95	32	-	-	-	-	-	
10	B-68-96	32	-	-	-	-	-	
10	B-68-97	32	-	-	-	-	-	
10	B-68-98	32	-	-	-	-	-	
10	B-68-99	32	-	-	-	-	-	
10	B-68-102	32	-	-	-	-	-	
10	B-68-103	32	-	-	-	-	-	
10	B-68-104	32	-	-	-	-	-	
10	B-68-105	32	-	-	-	-	-	
10	B-68-109	32	-	-	-	-	-	
10	B-68-110	32	-	-	-	-	-	
10	B-68-112	83	-	2	6	-	-	
10	B-68-121	64	-	-	-	-	-	
20	B-44-141,142,144	-	-	-	-	1	17	
10	B-44-148	-	36	-	-	-	-	
10	B-44-149	-	36	-	-	-	-	
10	B-44-182	-	36	-	-	-	-	
10	B-44-183	-	36	-	-	-	-	
10	B-44-189	-	36	-	-	-	-	
SUB TOTALS		1011	180	2	6			
CATTEGORY 20 TOTALS		0	0			1	17	
TOTALS		1191		2	6	1	17	

3

TRAFFIC CONTROL DETOUR SIGNS

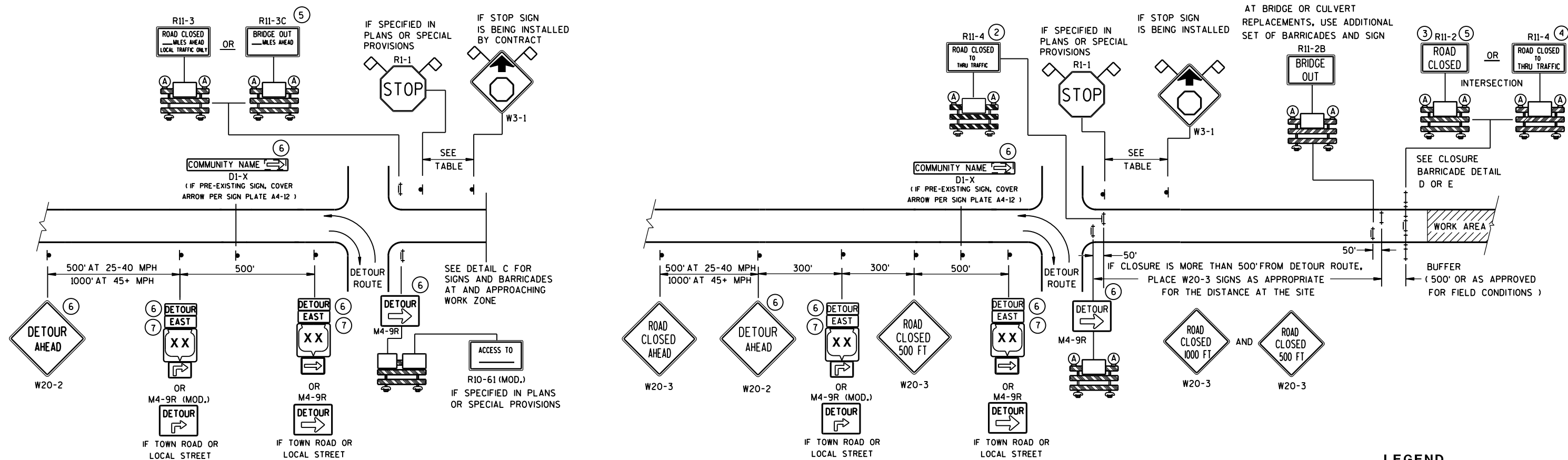
SIGN NUMBER	MESSAGE	NUMBER REQUIRED	DAYS NEEDED	643.3000 TOTAL DAYS
1	ROAD CLOSED AHEAD	1	3	3
2	LANE SHIFT 35 MPH	2	3	6
3	DETOUR AHEAD	1	3	3
4	DETOUR RIGHT ARROW	2	3	6
5	ROAD CLOSED AHEAD	1	3	3
6	END DETOUR	1	3	3
7	ARROW	1	3	3
8	DETOUR NORTH 45 ARROW	4	3	12
9	EAST 15 BEGIN RIGHT TURN LANE ARROW	3	3	9
10	DETOUR NORTH 45 ARROW	4	3	12
11	DETOUR NORTH 45	3	3	9
12	DETOUR	1	3	3
13	DETOUR NORTH 45 ARROW	3	3	9
14	DETOUR	1	3	3
15	DETOUR RIGHT ARROW	2	3	6
16	DETOUR ARROW	2	3	6
17	DETOUR ARROW	2	3	6
18	DETOUR	1	3	3
19	DETOUR NORTH 45 ARROW	4	3	12
20	DETOUR	1	3	3
21	DETOUR NEXT 5 MILES	1	3	3
22	DETOUR SOUTH 45 ARROW	4	3	12
23	DETOUR NORTH 45 ARROW	4	3	12
24	DETOUR SOUTH 45 ARROW	4	3	12
25	ARROW 15 MPH	2	3	6
26	DETOUR SOUTH 45 ARROW	4	3	12
27	DETOUR NORTH 45 ARROW	4	3	12
28	DETOUR SOUTH 45 ARROW	4	3	12
29	DETOUR NORTH 45 ARROW	4	3	12
30	DETOUR NORTH 45 ARROW	4	3	12
31	DETOUR NORTH 45 ARROW	4	3	12
32	DETOUR SOUTH 45 ARROW	4	3	12
33	DETOUR SOUTH 45 ARROW	4	3	12
34	DETOUR SOUTH 45 ARROW	4	3	12
35	DETOUR NORTH 45 ARROW	4	3	12
36	DETOUR NORTH 45 ARROW	4	3	12
37	DETOUR SOUTH 45 ARROW	4	3	12
38	DETOUR NORTH 45 ARROW	4	3	12
39	DETOUR SOUTH 45 ARROW	4	3	12
40	DETOUR NEXT 2.5 MILES	1	3	3
41	DETOUR NORTH 45 ARROW	4	3	12
42	DETOUR SOUTH 45 ARROW	4	3	12
43	DETOUR NORTH 45 ARROW	4	3	12
44	DETOUR NEXT 1.5 MILES	4	3	12

3

SIGN NUMBER	MESSAGE	NUMBER REQUIRED	DAYS NEEDED	643.3000 TOTAL DAYS
45	DETOUR 45 ARROW	3	3	9
46	DETOUR SOUTH 45 ARROW	4	3	12
47	45 DETOR AHEAD	2	3	6
48	CLINTONVILLE MARION	1	3	3
49	HORTONVILLE OSHKOSH	1	3	3
50	DETOUR NORTH 45 LEFT ARROW	4	3	12
51	DETOUR SOUTH 45 RIGHT ARROW	4	3	12
52	DETOUR TO 15 RIGHT ARROW	4	3	12
53	DETOUR NORTH ARROW	3	3	9
54	DETOR SOUTH	2	3	6
55	TO 15 ARROW	3	3	9
56	HOSPITAL ARROW	2	3	6
57	DETOR TO 15 LEFT ARROW	4	3	12
58	DETOUR SOUTH 45 ARROW	4	3	12
59	DETOUR NORTH 45 ARROW	4	3	12
60	HORTONVILLE OSHKOSH	1	3	3
61	ARROW	1	3	3
62-66	DETOUR ARROW	10	3	30
67	DETOUR	1	3	3
68	DETOUR ARROW	2	3	6
69	DETOUR	2	3	6
70	45 DETOUR AHEAD	2	3	6
71	NORTH 45 DETOUR AHEAD	3	3	9
72	ROAD CLOSED AHEAD	1	3	3
73	DETOUR AHEAD	1	3	3
74	DETOUR SOUTH 45 RIGHT ARROW	4	3	12
75	ROAD CLOSED 500 FEET	1	3	3
76	END DETOUR	1	3	3
77	DETOUR SOUTH 45 RIGHT KEEP RIGHT	4	3	12
78	DETOUR SOUTH 45 RIGHT ARROW	4	3	12
79	DETOUR NORTH LEFT ARROW	4	3	12
80	SOUTH 45	2	3	6
81	DETOUR AHEAD	1	3	3
82	ARROW	1	3	3
83	DETOUR SOUTH 45 ARROW	4	3	12
84	NORTH 45 RIGHT ARROW	3	3	9
85	DETOUR NORTH LEFT ARROW	3	3	9
86	DETOUR SOUTH 45 STRAIGHT ARROW	4	3	12
87	DETOUR NEXT 4 MILES	1	3	3
			TOTAL	702

Standard Detail Drawing List

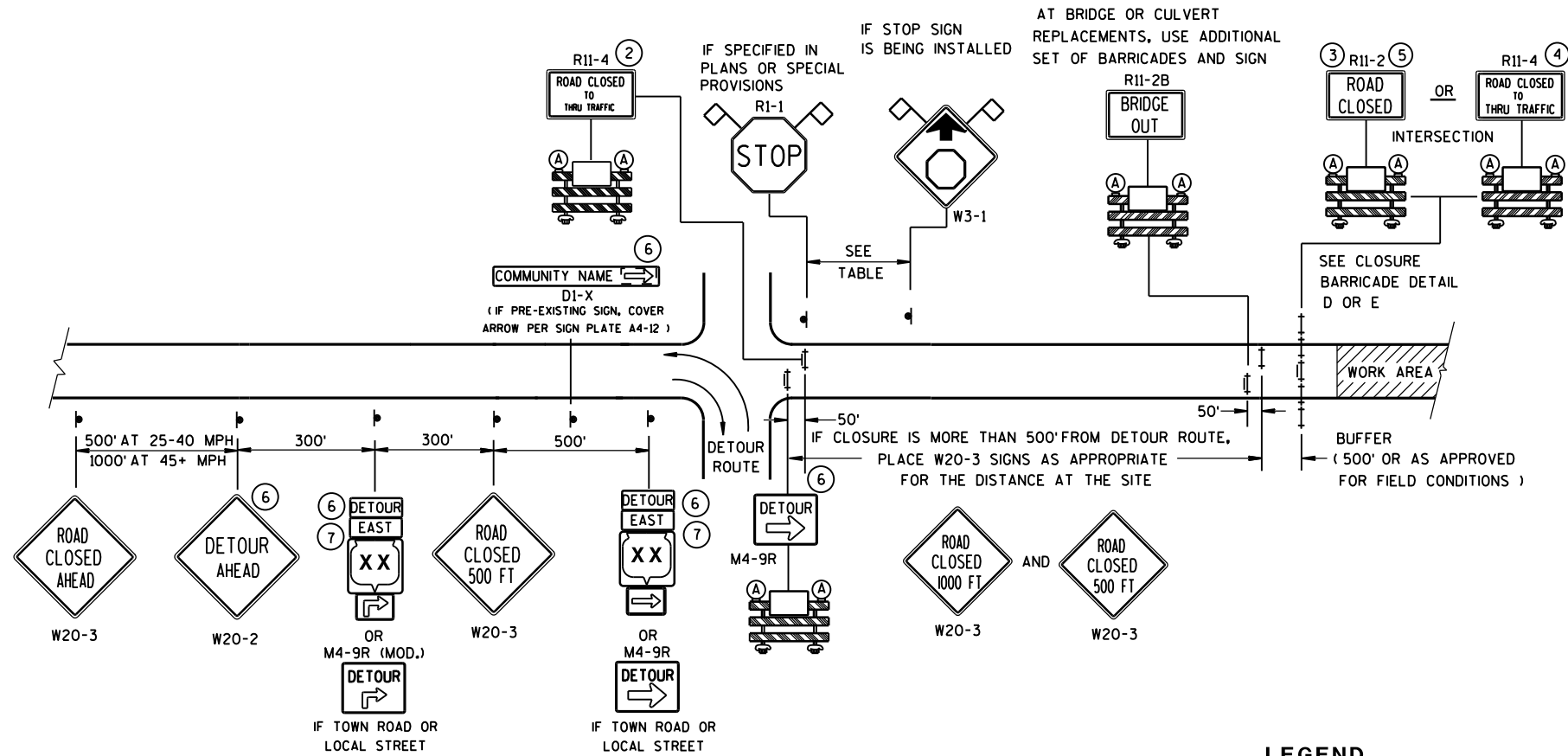
15C02-05A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05B	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-05C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-02	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-02	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C08-16A	PAVEMENT MARKING (MAINLINE)
15C12-04	TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)
15C31-01A	PAVEMENT MARKING (RAMPS AND GORES)
15D12-03	TRAFFIC CONTROL, LANE CLOSURE, SPEEDS GREATER THAN 40 M. P. H.
15D15-01	TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE
15D16-02	TRAFFIC CONTROL, EXIT RAMP CLOSURE



DETAIL A

MAINLINE CLOSURE WITH POSTED DETOUR

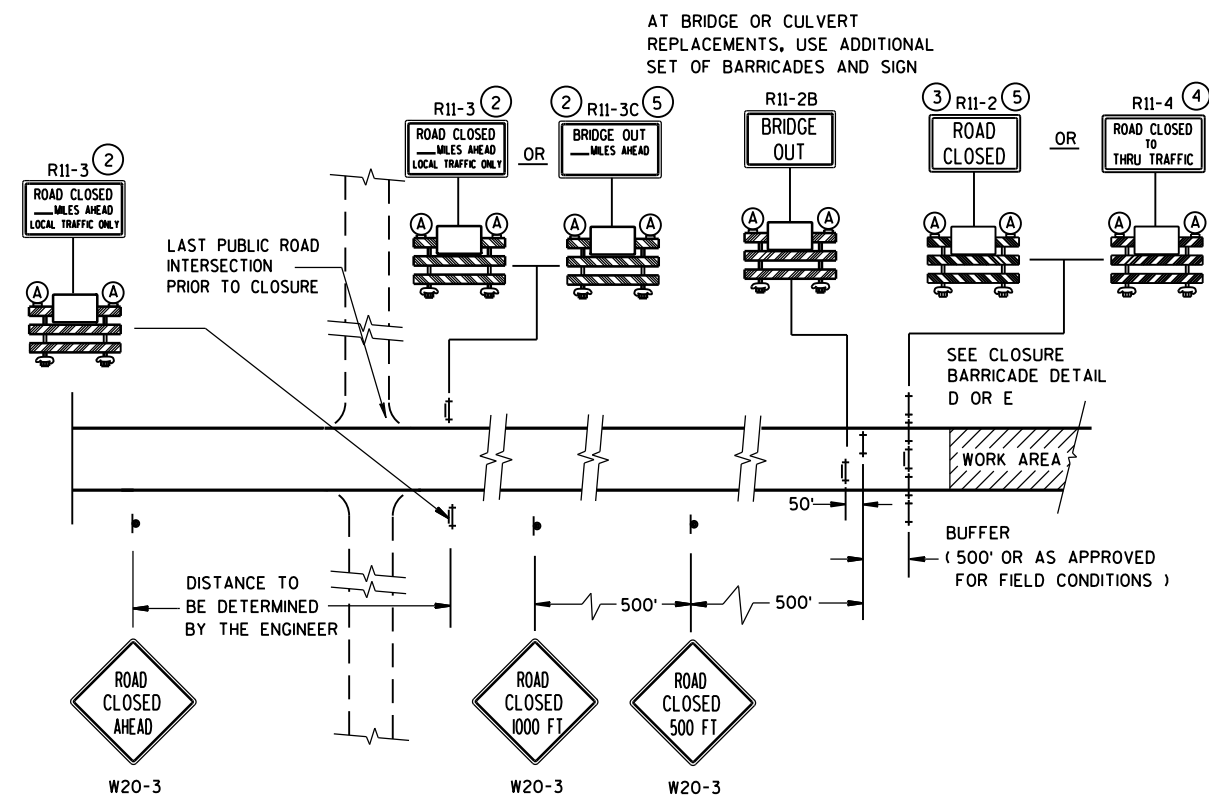
WORK ZONE GREATER THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B













MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN 1/2 MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



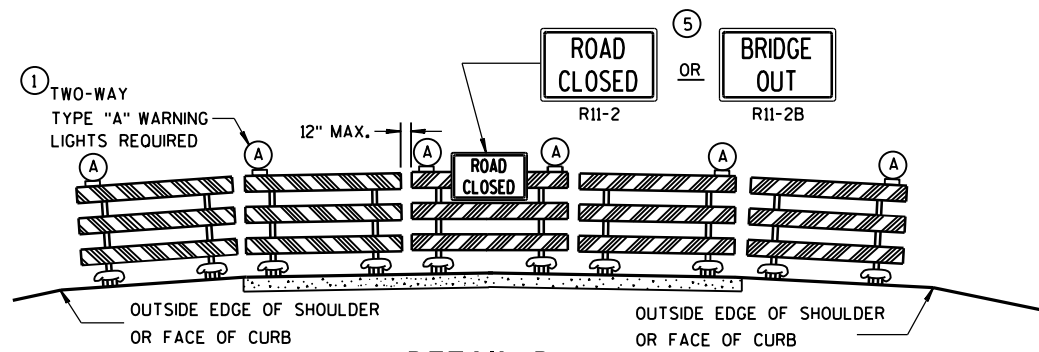
DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

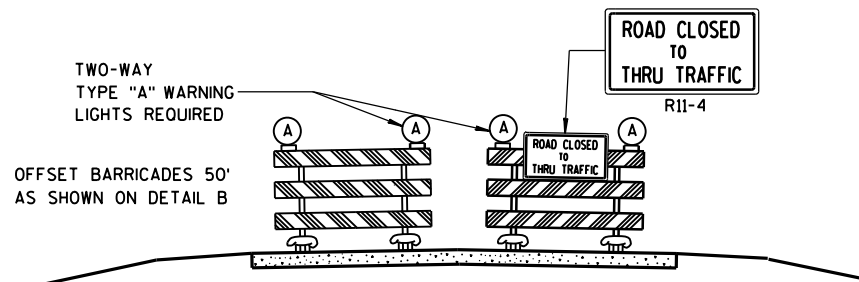
- # LEGEND
-  SIGN ON PERMANENT SUPPORT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  WORK AREA
-  M4-8
 M3-X
-  M1-4 OR  M1-5A OR  M1-6
-  M05-1 OR  M06-1
- FLAGS, 16" X 16" MIN., (ORANGE)

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
<u>8/2013</u> DATE	<u>/S/ Travis Feites</u> STATE TRAFFIC ENGINEER OF DESIGN



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2-SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, M4-9, R11-4 AND R10-61 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

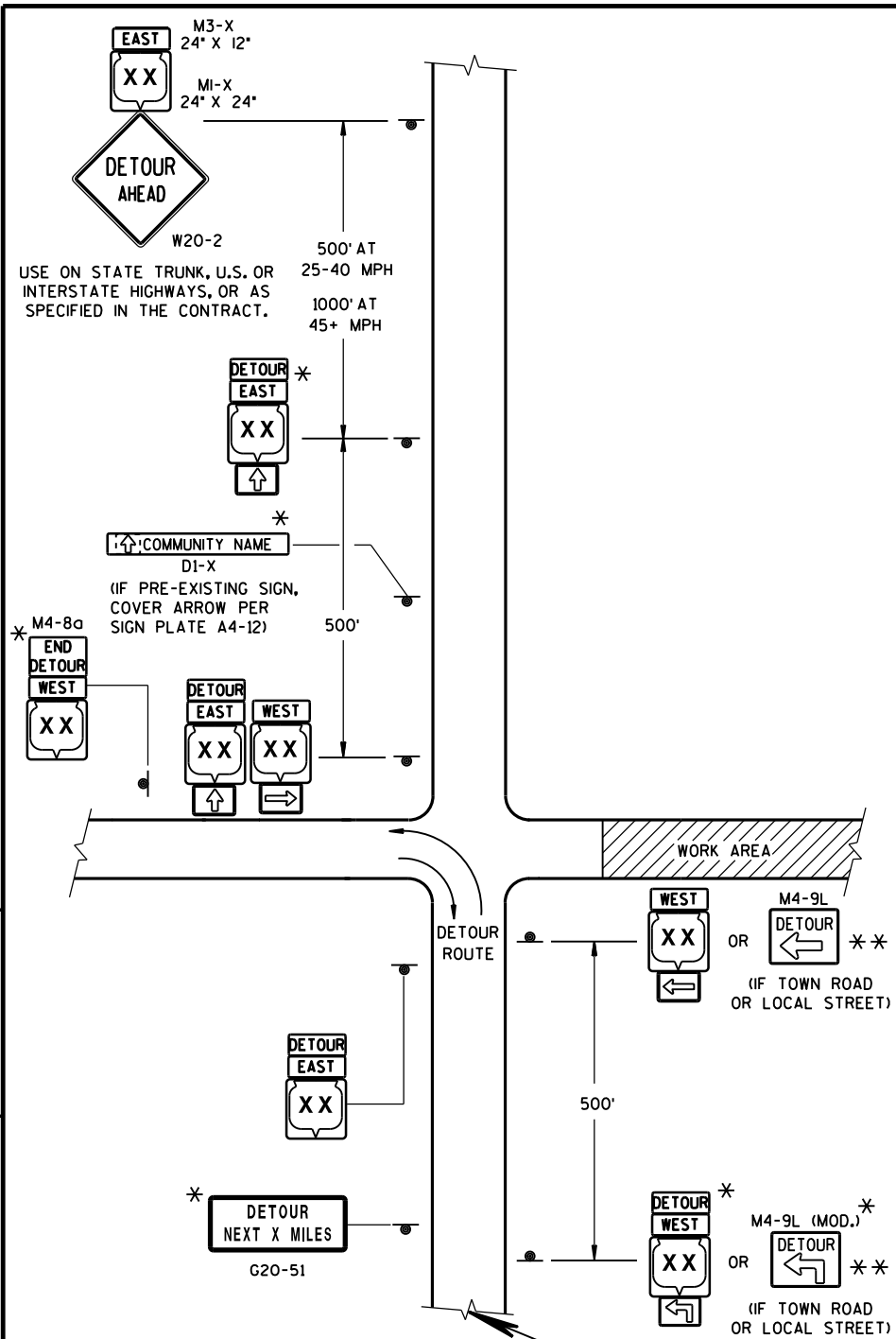
"WO AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11-2 SHALL BE 48" X 30".
- R11-3, R11-4 AND R10-61 SHALL BE 60" X 30".
- M4-9 SHALL BE 30" X 24".
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
- M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
- M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
- M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1-1 SHALL BE 36" X 36".

- 1 TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8-FOOT LIGHT SPACING).
- 2 THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT INTERSECTION.
- 3 FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL D.
- 4 FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE LANE CLOSURE BARRICADE DETAIL E.
- 5 FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11-2 AND R11-3 SIGNS.
- 6 INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- 7 "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



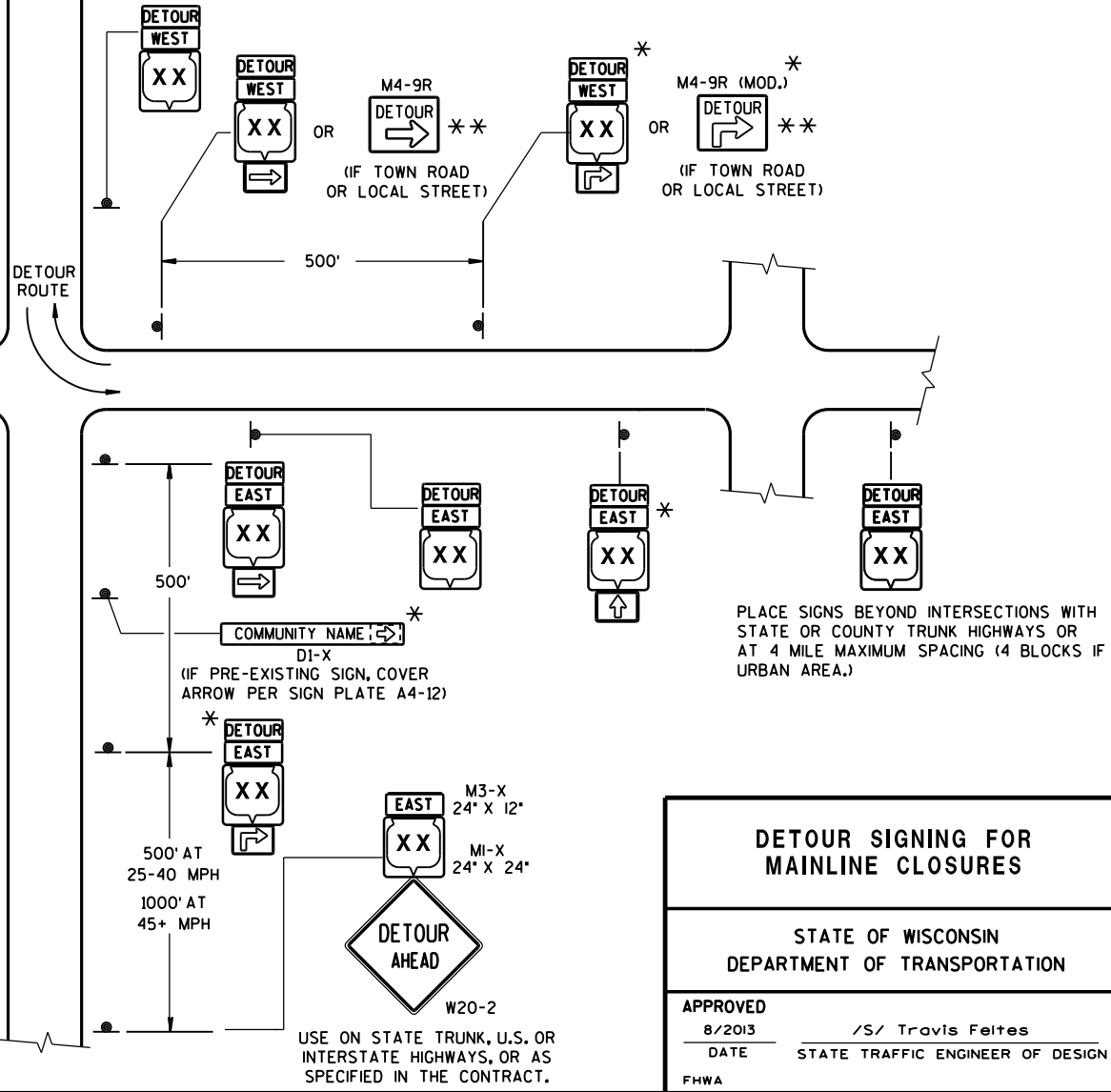
THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

MATCH POINT

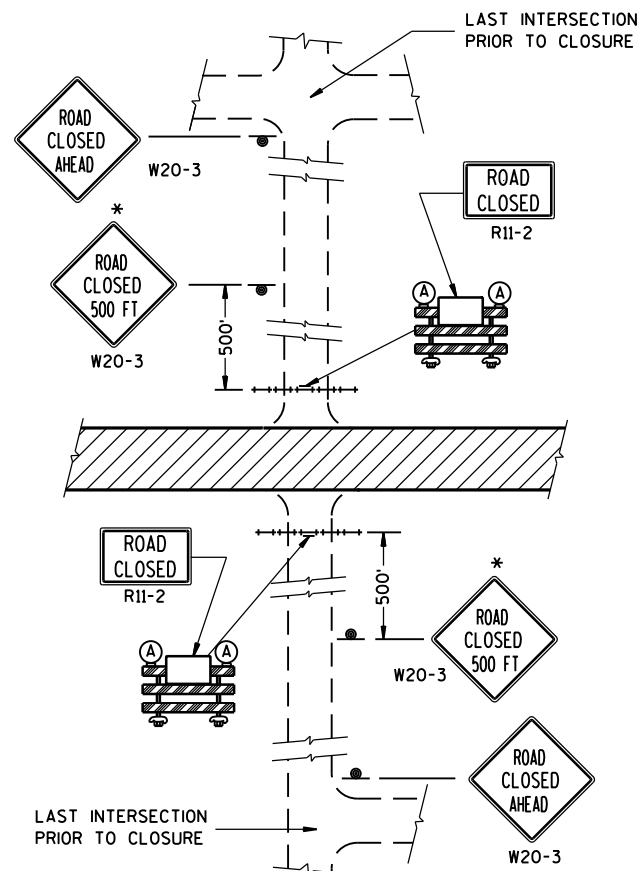
DETAIL F
DETOUR SIGNING

GENERAL NOTES

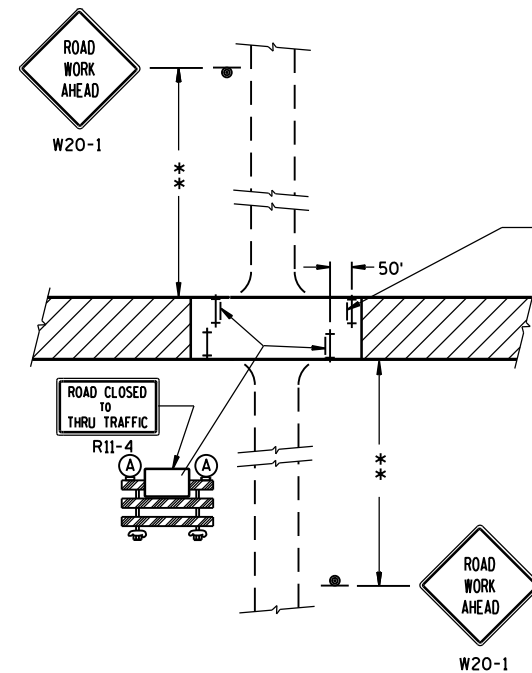
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS, MODIFY EXISTING SIGNS WHERE POSSIBLE.
- THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- "MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- SIGN SIZES SHALL BE AS FOLLOWS:
- M3-X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS.)
 - M4-8 SHALL BE 24" X 12". (30" X 15" IF NEEDED TO MATCH EXISTING SIGNS.)
 - M1-4, M1-5A, AND M1-6 SHALL BE 24" X 24". (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS.)
 - M05-1 AND M06-1 SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS.)
 - M4-9 SHALL BE 30" X 24".
 - M4-8a SHALL BE 24" X 18".
 - G20-51 SHALL BE 60" X 24".
 - W20-2 SHALL BE 48" X 48".
 - D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



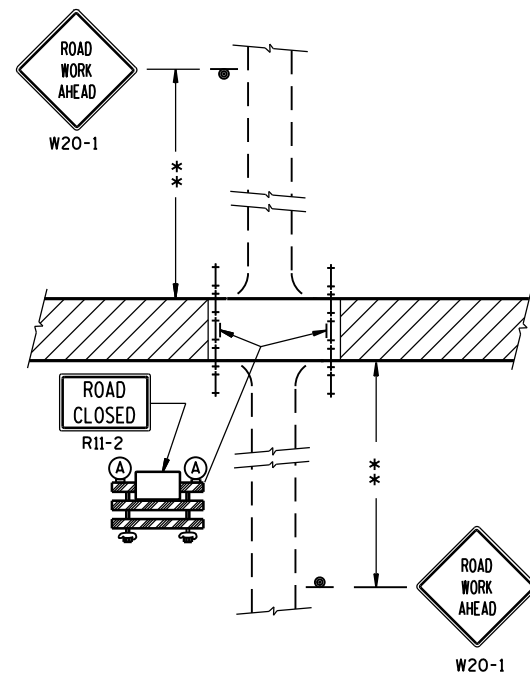
DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



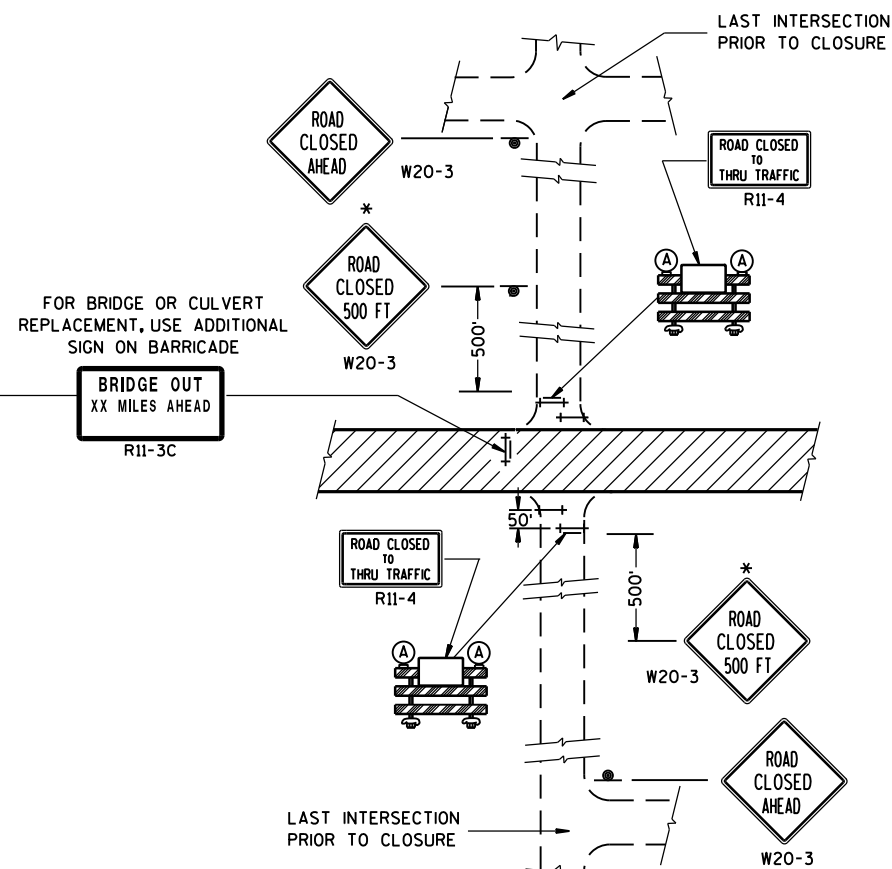
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED. CONTRACTOR, LOCAL BUSINESS AND RESIDENT ACCESS).



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT).



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION OR, FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL D FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3 AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

R11-2 SHALL BE 48" X 30".

R11-4 AND R11-3 SHALL BE 60" X 30".

*OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FT. OR LESS FROM THE WORK ZONE.

**500' MAX. OR AT LAST INTERSECTION WHICHEVER IS CLOSER.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- (A) TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

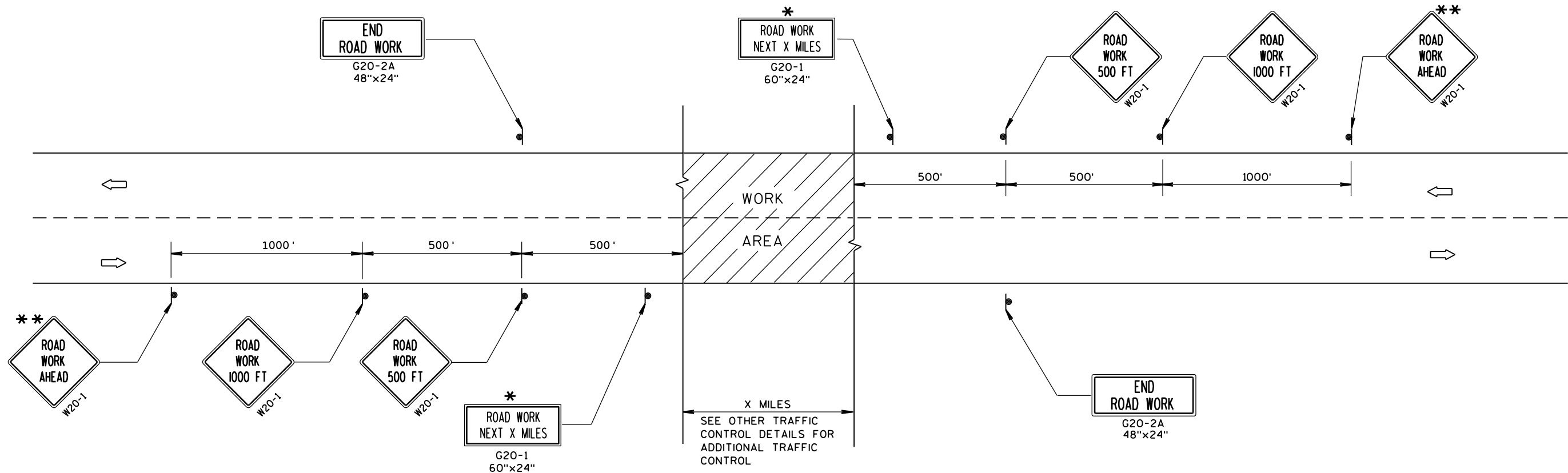
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

8/2013 /S/ Travis Feltes

DATE STATE TRAFFIC ENGINEER OF DESIGN

FHWA



TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

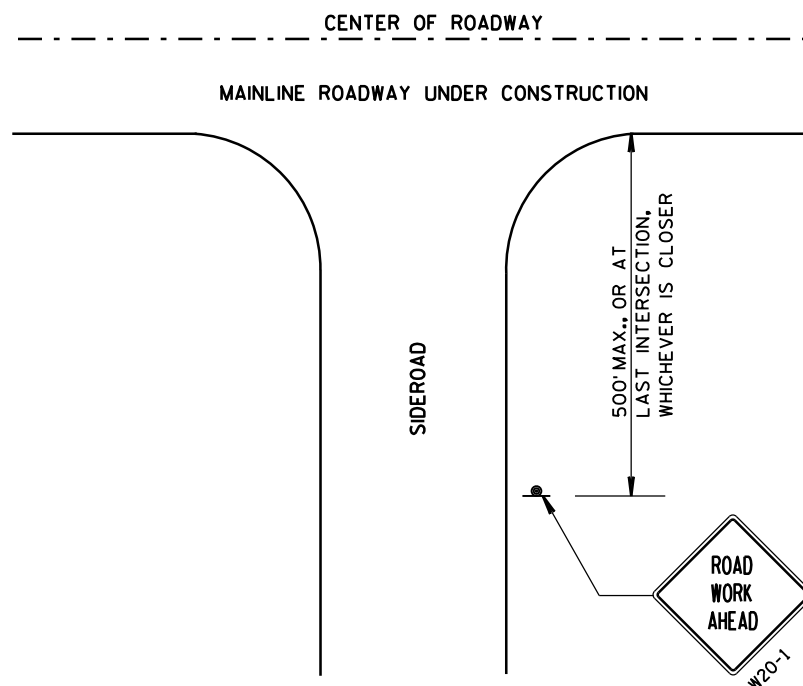
ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS RE-ESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

** PLACE ADDITIONAL W20-1 "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



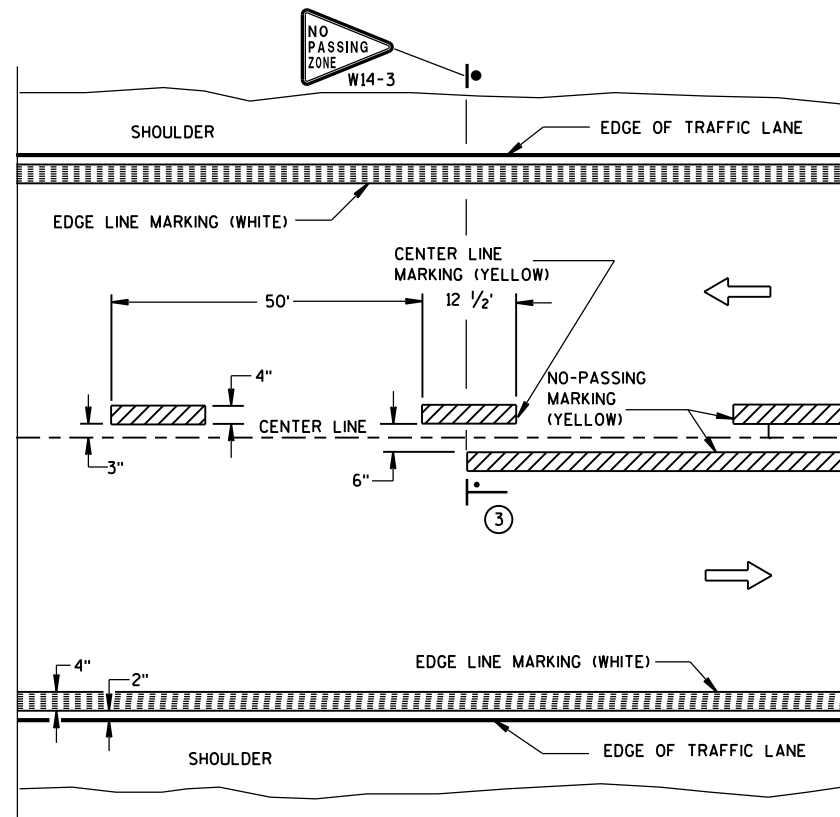
LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

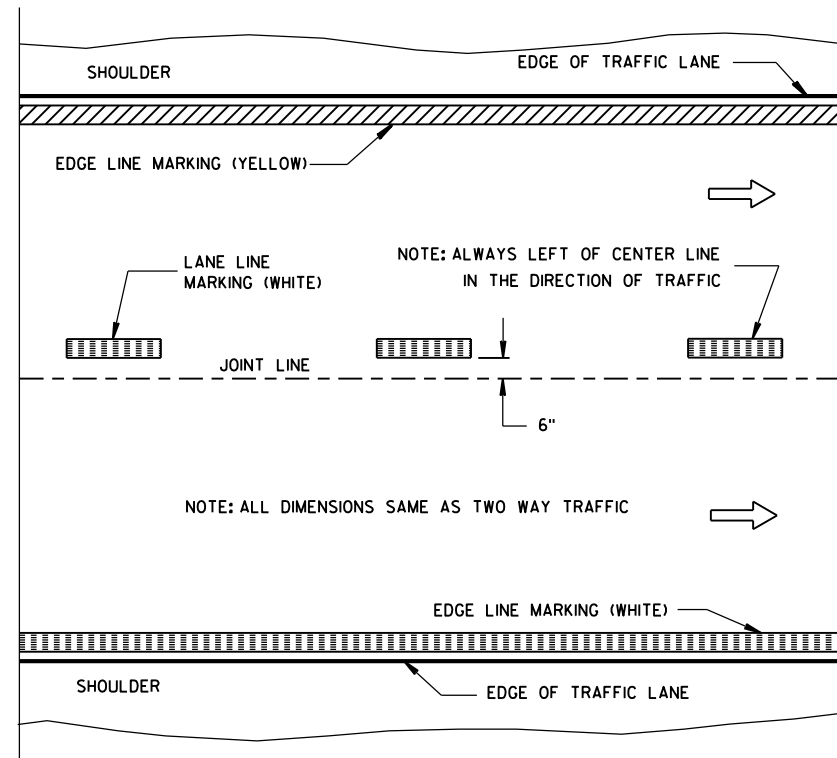
TRAFFIC CONTROL, ADVANCE
WARNING SIGNS 45 M.P.H.
OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

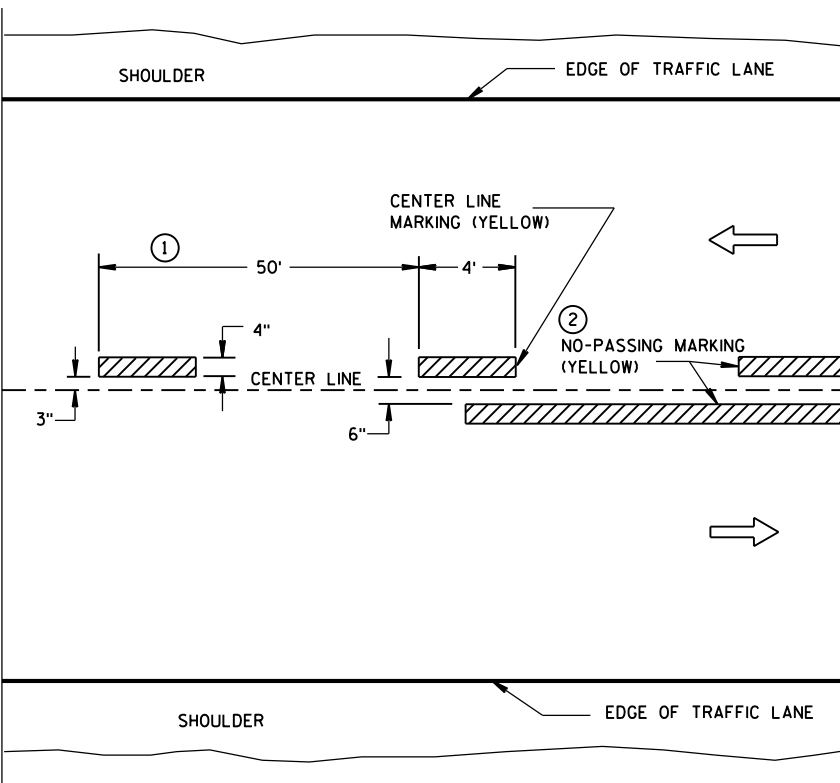


TWO WAY TRAFFIC

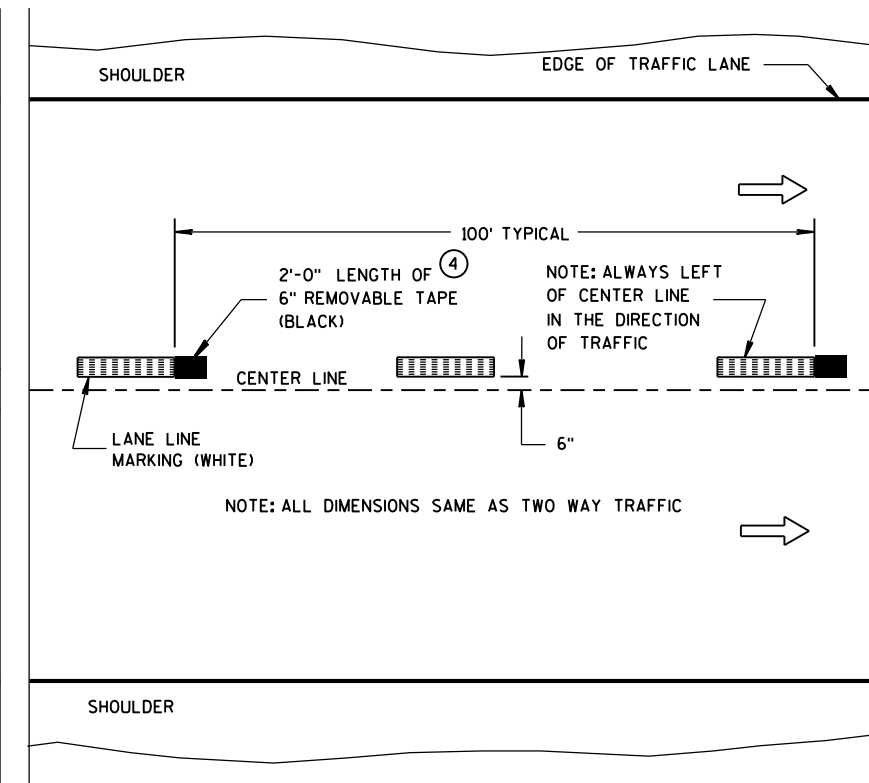


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY (INTERMEDIATE) PAVEMENT MARKING
(SHOWS CYCLE FOR TEMPORARY CENTER LINE OR TEMPORARY LANE LINE MARKING)

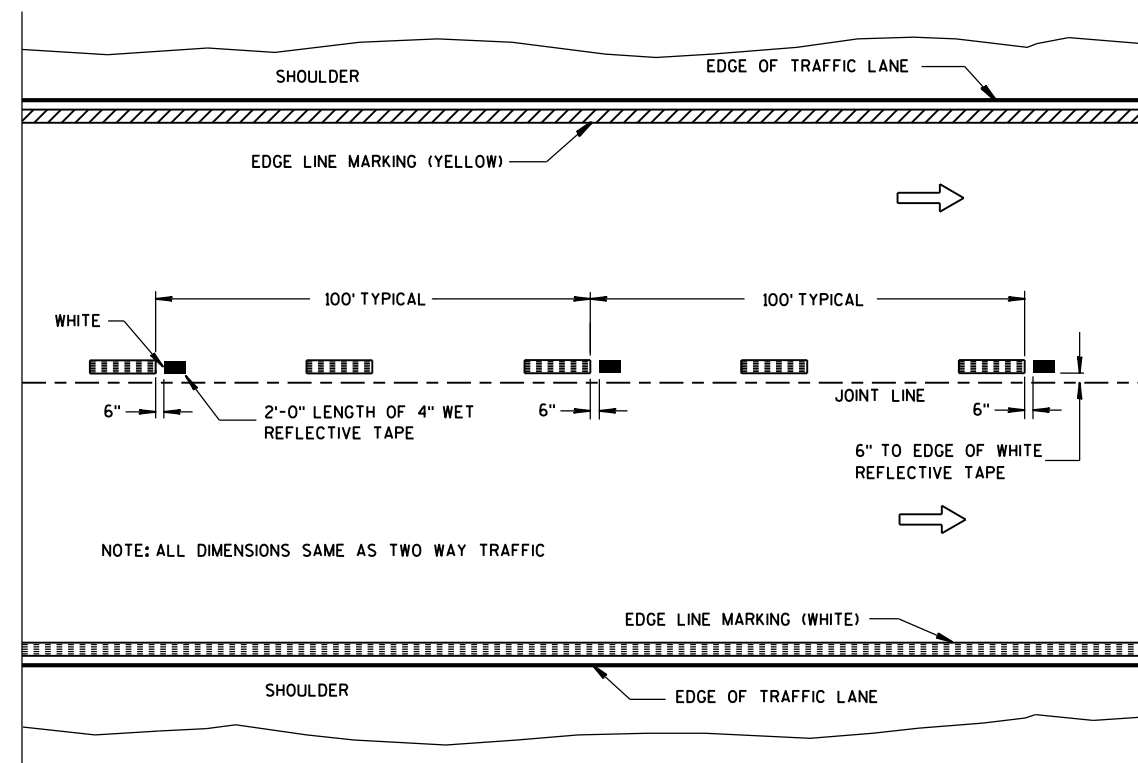
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① HALF CYCLE LENGTHS (25'±) WITH 2' MINIMUM STRIPE LENGTHS SHALL BE PROVIDED ON ROADWAYS (INCLUDING TEMPORARY TRAVELED WAYS) WITH REVERSE CURVATURE, CURVATURE OF OVER 5 DEGREES OR WHEN DIRECTED BY THE ENGINEER TO MARK UNUSUAL ALIGNMENT OF THE TRAVELED WAY.
- ② NO PASSING ZONE TEMPORARY PAVEMENT MARKING IS REQUIRED TO BE PLACED, WHERE APPROPRIATE, ALONG WITH CENTERLINE TEMPORARY PAVEMENT MARKING WHEN A SAME DAY PERMANENT PAVEMENT MARKING ITEM IS INCLUDED IN THE CONTRACT.
- ③ NO PASSING ZONE MARKINGS ARE PLACED ACCORDING TO "T" MARKINGS. IF EXISTING NO PASSING ZONE W14-3 SIGNS ARE BEYOND 50 FEET IN EITHER DIRECTION, THE SIGNS SHALL BE MOVED TO THE "T" MARKINGS.
- ④ CONCRETE ONLY.

NOTE

ARROW SYMBOL (→) SHOWS DIRECTION OF TRAVEL



WET REFLECTIVE TAPE SUPPLEMENT TO
SPRAYED OR NON WET REFLECTIVE TAPE LANE LINE

LEGEND

- "T" MARKING
- POST MOUNTED SIGN

PAVEMENT MARKING
(MAINLINE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
5-13-2013
DATE
FHWA

/S/ Travis Feltes
STATE TRAFFIC ENGINEER

LEGEND

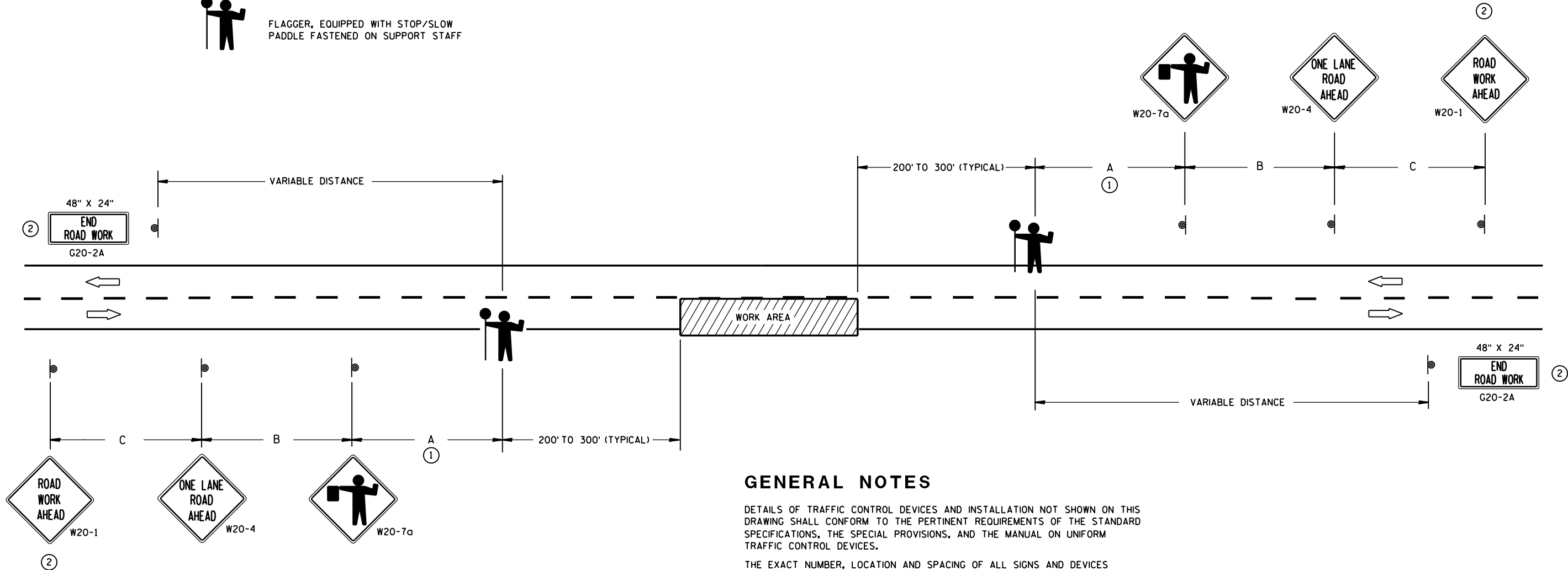
-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

SIGN SPACING TABLE

SPEED LIMIT	SIGN SPACING A,B,C
25-35 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF THE "BE PREPARED TO STOP" SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7a AND W20-4 SIGNS. A 500' TYPICAL SPACING SHALL BE PROVIDED BETWEEN THE SIGNS.



GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES (AND THE LOCATION OF ALL FLAGGERS) SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT, COVER OR REMOVE ALL TEMPORARY TRAFFIC CONTROL SIGNS.

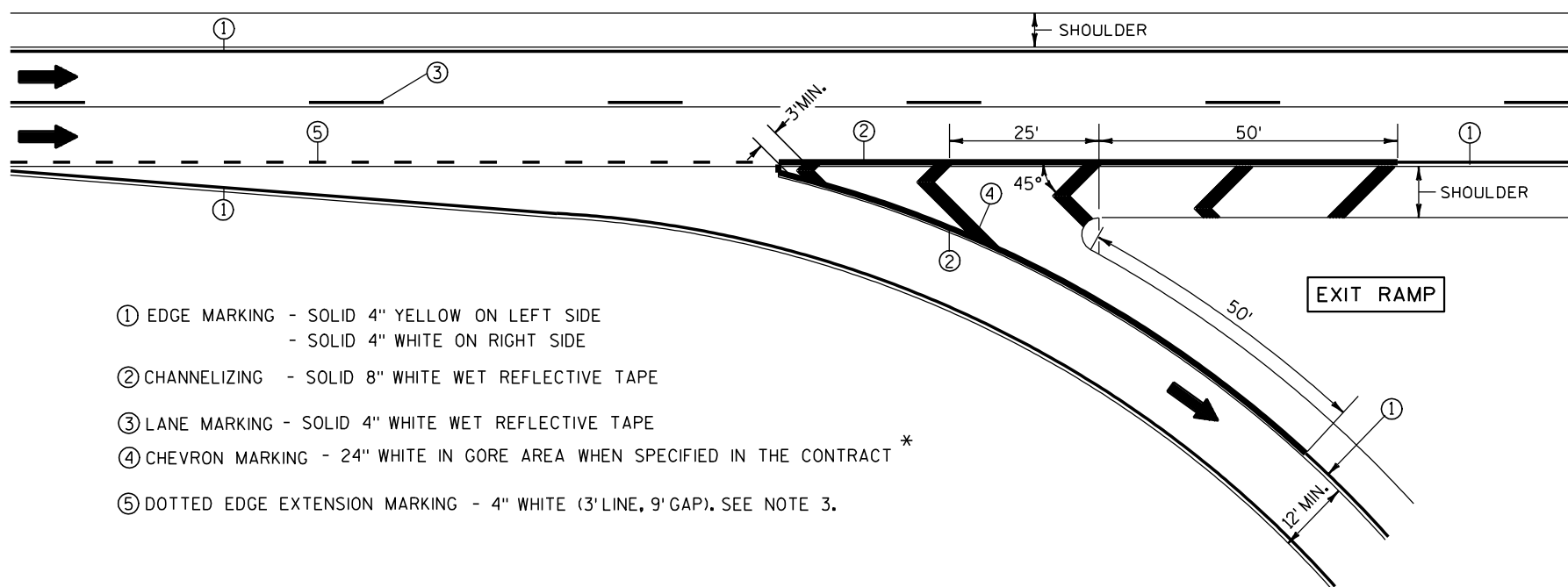
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

- ① FOR A MOVING WORK OPERATION, SIGNING FOR BOTH DIRECTIONS SHALL BE REESTABLISHED (AS SIMULTANEOUSLY AS PRACTICAL) AT APPROXIMATELY 3500 FOOT INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

TRAFFIC CONTROL FOR LANE CLOSURE (SUITABLE FOR MOVING OPERATIONS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8/2013 /S/ Travis Feltes
DATE STATE TRAFFIC ENGINEER OF DESIGN
FHWA

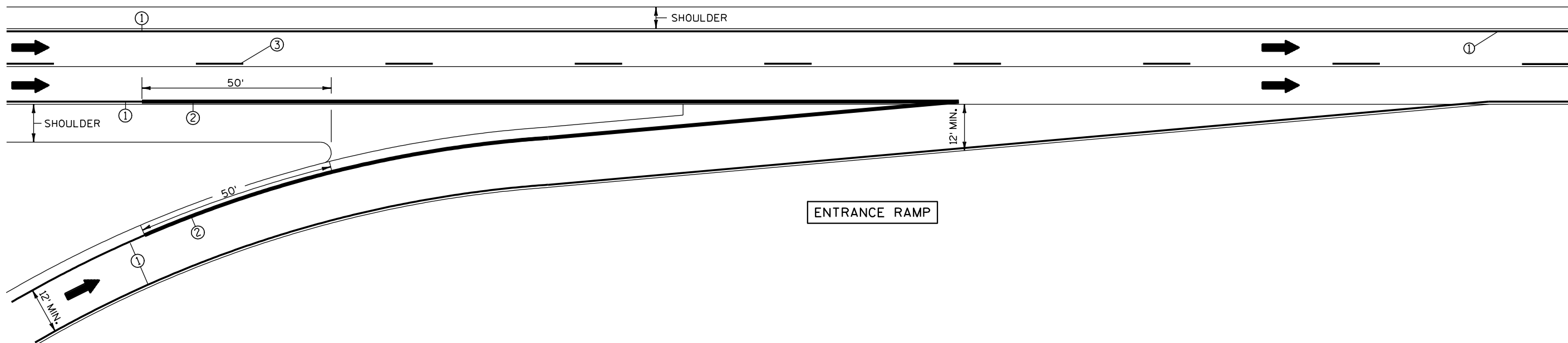


- ① EDGE MARKING - SOLID 4" YELLOW ON LEFT SIDE
- SOLID 4" WHITE ON RIGHT SIDE
- ② CHANNELIZING - SOLID 8" WHITE WET REFLECTIVE TAPE
- ③ LANE MARKING - SOLID 4" WHITE WET REFLECTIVE TAPE
- ④ CHEVRON MARKING - 24" WHITE IN GORE AREA WHEN SPECIFIED IN THE CONTRACT *
- ⑤ DOTTED EDGE EXTENSION MARKING - 4" WHITE (3' LINE, 9' GAP). SEE NOTE 3.

NOTES:

- 1. ARROWS SHOWN ON THIS MARKING PLAN DESIGNATE TRAFFIC FLOW, AND SHALL NOT BE TAKEN AS PROPOSED PAVEMENT MARKINGS.
- 2. PLACE WHITE EDGE OF TAPE 6" LEFT FROM JOINT.
- 3. 3' LINE 9' GAP, EXCEPT RETRACE THE EXISTING LINE-GAP PATTERN WHERE EXISTING MARKINGS ARE IN PLACE.
- 4. RETRACE EXISTING DIAGONAL MARKINGS.

* REFER TO DESIGN NOTES.



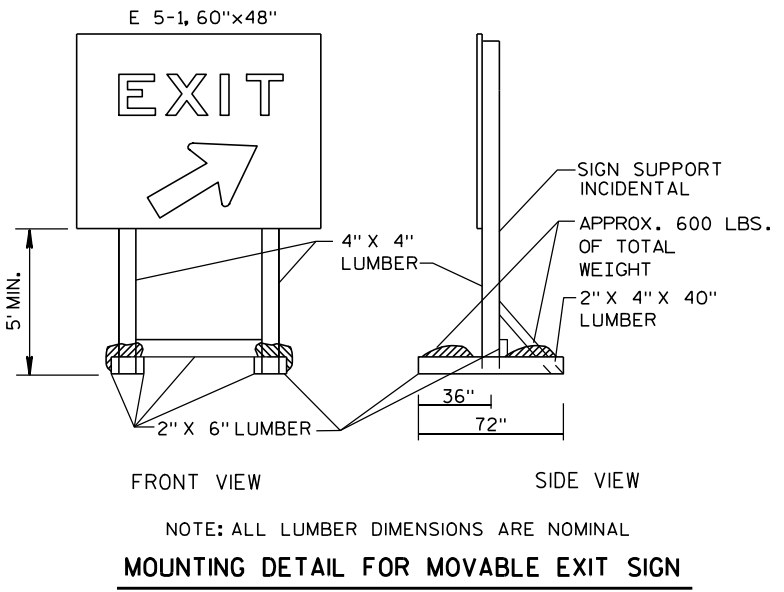
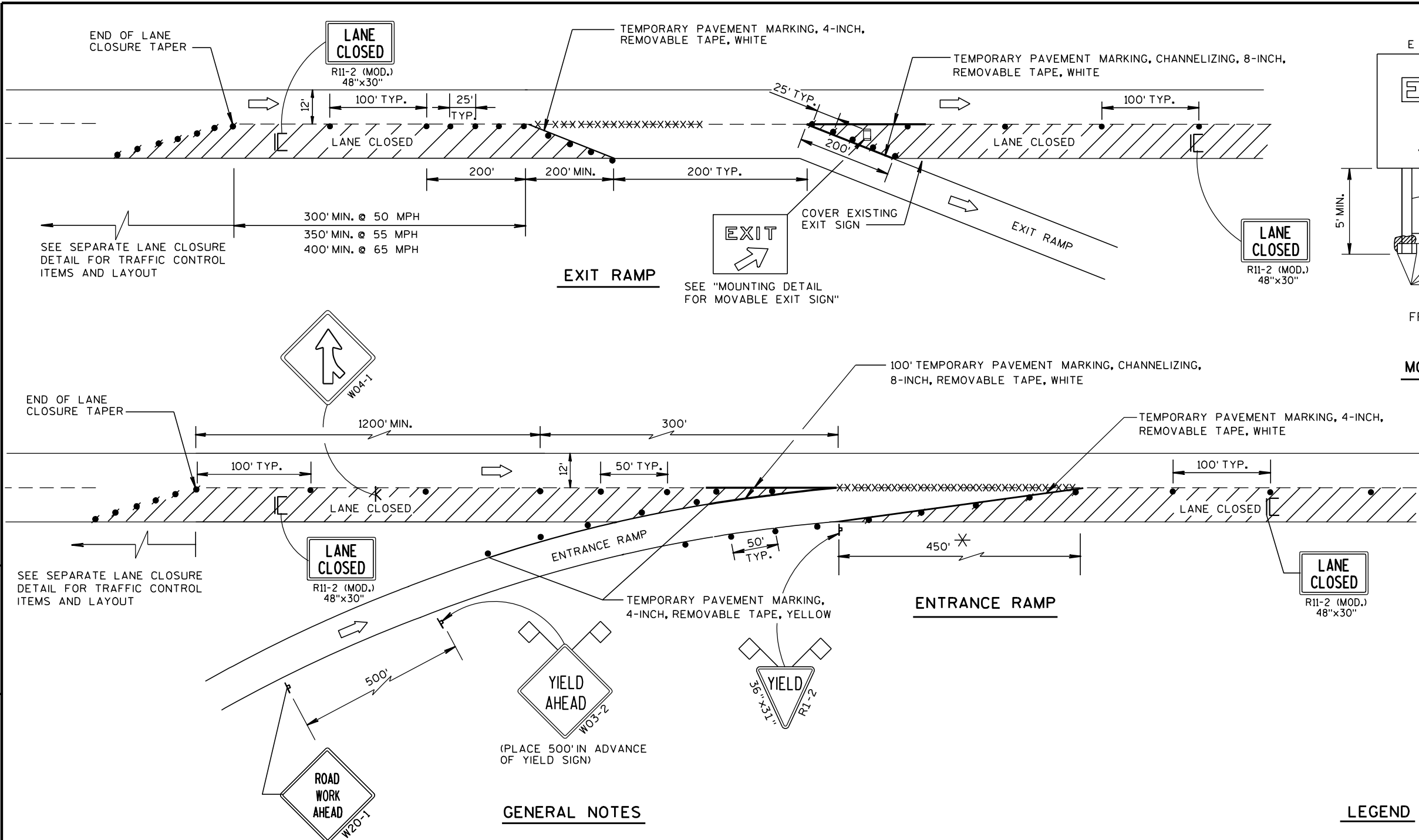
PAVEMENT MARKING
(RAMPS AND GORES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

- S.D.D. 15 D 12-3**

6



GENERAL NOTES

THE INSTALLATIONS SHOWN ON THIS SHEET ARE TYPICAL EXAMPLES AND ARE NOT INTENDED TO REPRESENT ANY PARTICULAR RAMP. AT SPECIFIC FIELD LOCATIONS, SIMILAR INSTALLATIONS SHALL BE USED AND ADJUSTED TO THE GEOMETRICS OF THE RAMP AS COORDINATED WITH THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SEE SEPARATE LANE CLOSURE DETAIL FOR TYPICAL SPACING OF TYPE III BARRICADES AND R11-2 (MOD.) "LANE CLOSED" SIGNS.

YIELD SIGN AND WARNING SIGNS ON ENTRANCE RAMP ARE ALSO APPROPRIATE FOR CLOSURE OF THE MAINLINE LEFT LANE. OMIT THE YIELD SIGN IF MORE THAN ONE LANE REMAINS OPEN ON THE MAINLINE AND THE RAMP TAPER IS AT LEAST AS LONG AS THE NORMAL ENTRANCE RAMP TAPER AT THE SITE.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, OR THAT WILL BE PLACED IN A CLOSED LANE, MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF INDICATED IN MISCELLANEOUS QUANTITIES, SUBSTITUTE FLEXIBLE TUBULAR MARKERS FOR DRUMS IN THE GORE BETWEEN THE ENTRANCE RAMP AND MAINLINE TRAFFIC.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF LANE CLOSURE IS TO BE IN PLACE FOR 7 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

* LENGTH OF OPENING MAY BE REDUCED TO 150 FEET DURING STAGING OF WORK IN IMMEDIATE AREA OF RAMP TAPER.

LEGEND

- POST MOUNTED SIGN
- SIGN ON PORTABLE SUPPORT
- TRAFFIC CONTROL, DRUM
- TRAFFIC CONTROL, DRUM WITH WARNING LIGHT, TYPE C (STEADY-BURN)
- REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
- TYPE III BARRICADE (8' EQUIVALENT) WITH SIGN
- FLAGS, 16"x16" MIN., ORANGE
- DIRECTION OF TRAFFIC FLOW

TRAFFIC CONTROL, EXIT AND ENTRANCE RAMP WITHIN LANE CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 5/24/2000 DATE	/S/ Chester J. Spang CHIEF SIGNS AND MARKING ENGINEER
FHWA	

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC

GENERAL NOTES

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

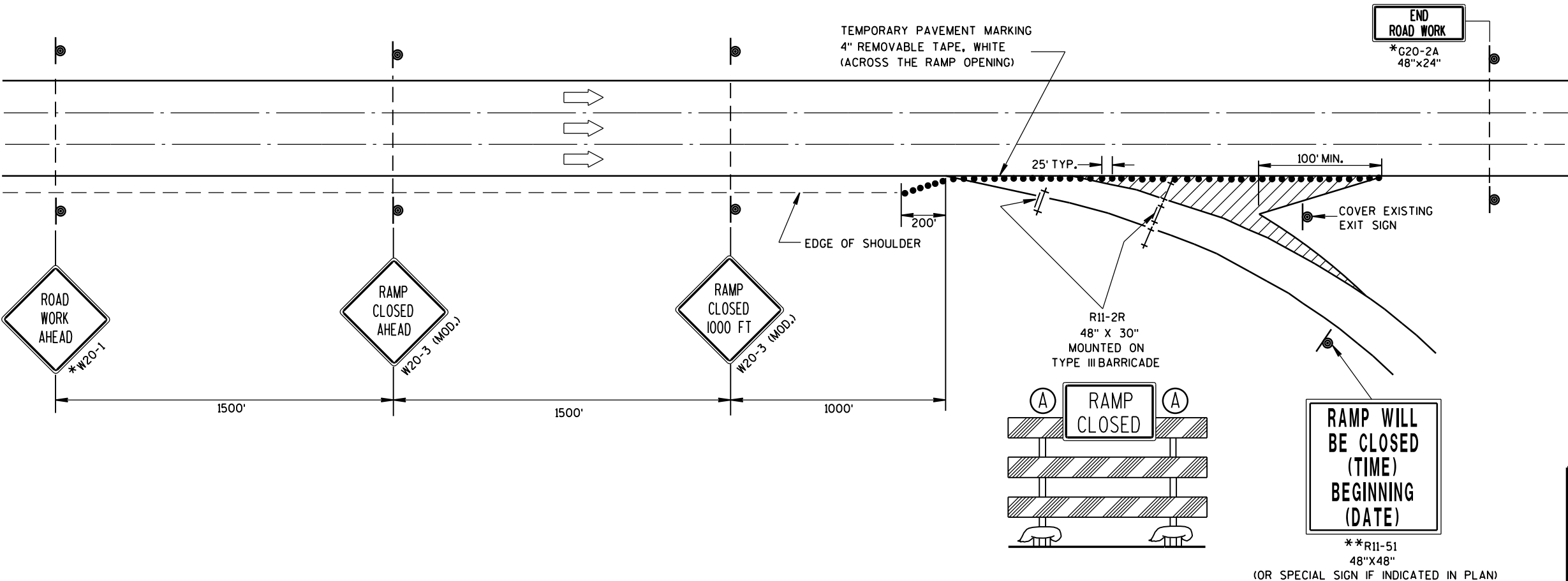
PLACE TEMPORARY PAVEMENT MARKING, REMOVABLE TAPE IF RAMP CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROPOFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN THE WORK IS NOT IN PROGRESS.

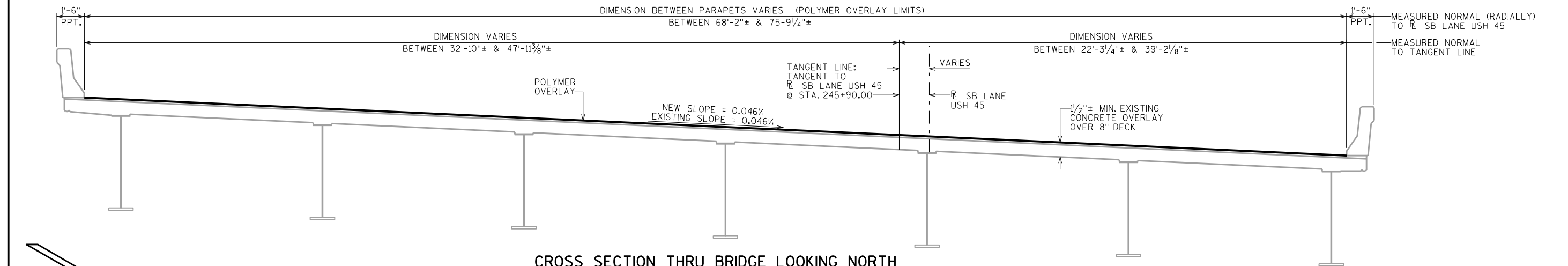
WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12-HOUR DURATION.

*W20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

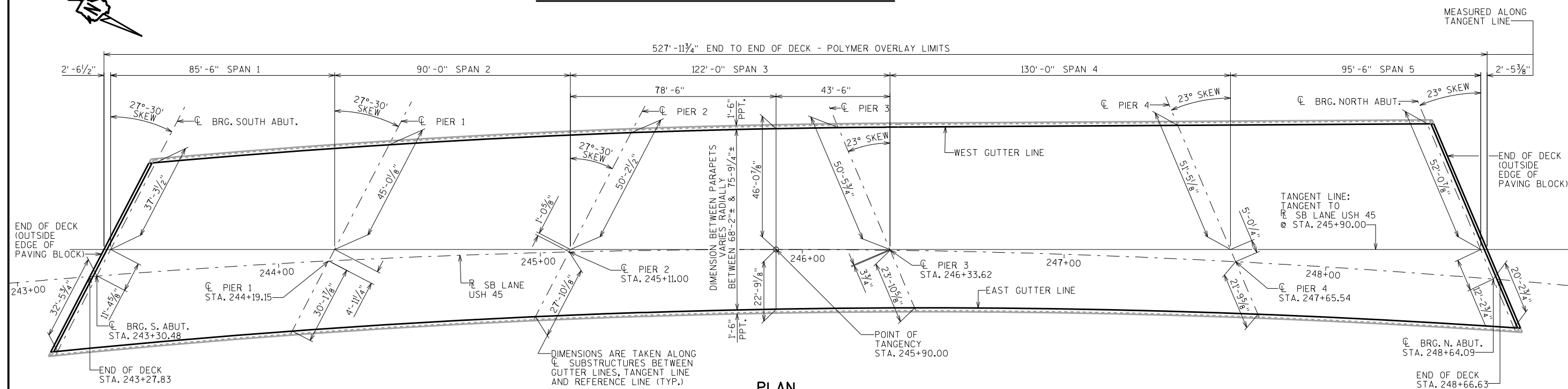
** PLACE "RAMP WILL BE CLOSED" SIGN 10 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



TRAFFIC CONTROL, EXIT RAMP CLOSURE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/2013 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



CROSS SECTION THRU BRIDGE LOOKING NORTH



PLAN

STRUCTURES DESIGN CONTACTS:
KELLY YOUNG (608) 267-7943
AARON BONK (608) 261-0261

DESIGN DATA

LIVE LOAD:

INVENTORY RATING: HS-27
OPERATIONAL RATING: HS-45
MAXIMUM STANDARD PERMIT VEHICLE LOAD: 250 KIPS

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

DIMENSIONS SHOWN ARE BASED ON THE EXISTING ORIGINAL STRUCTURE PLANS.

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
509.5100.S	POLYMER OVERLAY	SY	4086


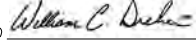
LIST OF DRAWINGS

1. POLYMER OVERLAY

CURVE DATA

R SB LANE USH 45

P.I. = STA. 252+72.11
 Δ = 34°-13'-23"
D = 01°-43'-06.62"
T = 1026.42'
L = 1991.44'
R = 3334.05'
S.E. = 0.046%
P.C. = STA. 242+45.69
P.T. = STA. 262+37.13

NO.	DATE	REVISION	BY
 Plans Prepared By WISDOT BUREAU OF STRUCTURES			
ACCEPTED 		10/9/14 DATE	
CHIEF STRUCTURES DESIGN ENGINEER			
STRUCTURE B-44-144			
USH 45 OVER STH 54 & EMBARRASS RIVER			
COUNTY	OUTAGAMIE	CITY	NEW LONDON
DESIGN SPEC.	REHABILITATION	N/A	
DESIGNED BY	DESIGN CK'D.	DRAWN BY	PLANS CK'D.
KCY		JPH	AMB
POLYMER OVERLAY			SHEET 1 OF 1

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>